Public Involvement Meeting Handout

Oregon/Jackson Street Bridge, City of Oshkosh Fox River Bridge & Approaches Winnebago County

Project ID: 4994-07-00



April 27, 2016 5 p.m. – 7 p.m. Oshkosh City Hall, Room 404 215 Church Avenue

Purpose of the meeting

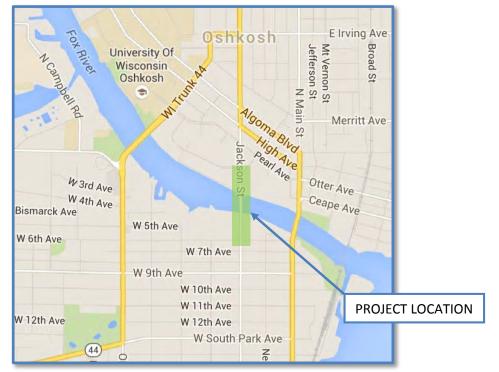
The Wisconsin Department of Transportation (WisDOT) is in the initial stage of a study of the Oregon/Jackson Street Bridge that will first determine if there is a need for improvements, and if so, develop alternatives that address the needed improvements, evaluate the impacts of those alternatives and recommend an alternative that best addresses the needs and minimizes impacts.

This first public involvement meeting is being held to introduce the study to the public and to obtain feedback from you on issues that will assist WisDOT in establishing a study purpose and need.

This public involvement meeting is an open house format. A brief, formal introduction of the study will be presented at 5:10 p.m. After the presentation, the public is invited to view the exhibits. WisDOT staff and their consultants will be available throughout the evening to answer any detailed questions you may have as you view the exhibits.

During the open house we encourage you to:

- Introduce yourself to study team members
- Understand and provide input on the purpose and need for the study
- Discuss the range of alternatives to be studied
- Review the study schedule
- Provide comment(s) on any issue(s) you feel are important to address
- Ask any questions you may have about the study



Project information

Oregon/Jackson Street Bridge location map

The study team has begun the environmental documentation for this project. Historical and archaeological investigations are underway – if you have knowledge of historical or archaeological significant features along the corridor contact the project manager listed at the end of this handout.

The study team has completed an inspection report on the condition of the existing structure. While the bridge is safe for use, there are numerous deficiencies with the existing bridge that need to be addressed. As the study moves forward and it is determined that improvements are required, any bridge improvement alternatives must address structural and geometric deficiencies of the existing bridge, maintain safe access and passage for all users (including bicyclists, pedestrians, individuals with disabilities, waterway traffic and motor vehicles), meet transportation demand, and comply with all state, regional and local plans.

A summary of the report findings is presented below.

Bridge Deficiencies

- Movable bridge machinery shows signs of wear and corrosion and needs replacement
- Deterioration of tack welds and steel plates require repair
- Cracking and spalling on concrete surfaces of bridge piers
- Sidewalks are less than the standard widths
- Approach spans and sidewalks need repair
- Vertical profiles of roadway approaches are below current design standards

Bridge Operations

- Bridge tender has poor visibility to monitor vehicles and pedestrians when raising the bridge
- Low vertical clearance bridge had 400 more openings in 2015 than adjacent bridges

Safety

- Vehicle crash rate is above statewide average for similar roadways
- Bridge railing of bascule span is offset into sidewalk, creating potential hazards for pedestrians and bicyclists
- Narrow lane and sidewalk widths do not provide safe bicycle accommodations
- Riverwalk pedestrians and bicycle traffic are required to cross road at grade (cannot cross under bridge), creating a mid-block at-grade pedestrian crossing
- Steel grid panels in deck are offset and present a hazard to bicyclists
- Approach sidewalks have settled and present a tripping hazard

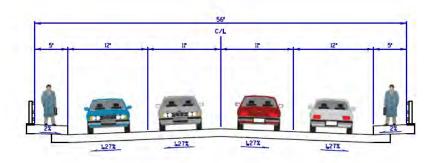
Alternatives that will be evaluated

NO-BUILD - Base alternative 'do-nothing'

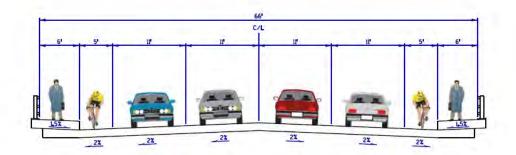
REHABILITATION – Repair structure deficiencies (no change to bridge width or clearances)

FULL REPLACEMENT – Replace entire bridge on existing or new alignment

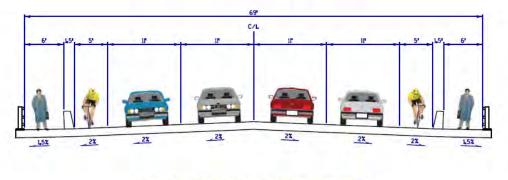
Note: See typical sections on the next page of this handout.



REHABILITATED EXISTING TYPICAL SECTION



RECONSTRUCTED TYPICAL SECTION WITHOUT BARRIER



RECONSTRUCTED TYPICAL SECTION WITH BARRIER

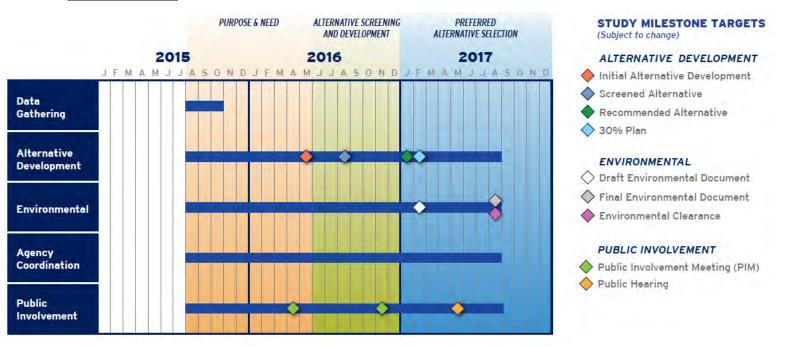
Proposed traffic impacts

Traffic impacts and potential detours will be addressed at future meetings. It is too early in the process to determine how traffic will be impacted. WisDOT understands minimizing traffic impacts is important along any corridor and will provide updates on any proposed impacts and/or detours as they are determined.

Construction is currently scheduled to begin in 2022 and could extend between 1-2 years depending on the preferred alternative that is selected (see '*Potential Project Schedule*' in this handout).

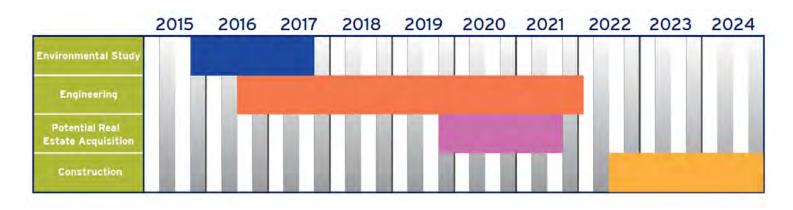
Project update/next steps

The study team will collect and review all comments received from tonight's meeting. These comments will assist us in completing the study's purpose and need statement and in developing improvement alternatives as needed. We will present the purpose and need and more detailed improvement alternatives at our next public involvement meeting. This next meeting is planned for the fall of 2016. Notice of meeting details will be published and invitations will be sent to those on the project mailing list.



Study Schedule

Potential Project Schedule



Public input/comments

We encourage you to talk to the project representatives and ask them questions. Attached to this handout is a sheet for your written comments and input regarding this study. Please mail any written comments about the study before May 13, 2016 or leave them in the comment box tonight. You can also email your comments to the project manager listed below.

Your comments assist us in developing a project that will serve the needs of the traveling public as well as the needs of the local community. Your input is welcome and appreciated throughout the study process.

For more information, please contact:

Bill Bertrand, P.E., Project Manager Wisconsin Department of Transportation, Northeast Region 944 Vanderperren Way Green Bay, WI 54304-5344 (920)360-3124 william.bertrand@dot.wi.gov

Chuck Karow, P.E., Project Supervisor Wisconsin Department of Transportation, Northeast Region 944 Vanderperren Way Green Bay, WI 54304-5344 (920)492-5997 <u>charles.karow@dot.wi.gov</u>

Public Involvement Meeting Comment Form

Project ID: 4994-07-00 Oregon/Jackson Street Bridge, City of Oshkosh Fox River Bridge & Approaches Winnebago County

Wednesday, April 27, 2016

Please place this form in the comment box or mail by May 13, 2016 to the address on the back of this sheet. Comments can also be e-mailed to <u>william.bertrand@dot.wi.gov</u>. Your comments assist us in developing a project that will serve the needs of the traveling public as well as the needs of the local community. Your input is welcome and appreciated throughout the design process.

Name: _____

Address: _____

Daytime Phone Number (optional): _____

Email Address	(optional):		
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Please Print Comments (attach additional sheets if necessary)

The information in this document including names, addresses, phone numbers, e-mail addresses, and signatures is not confidential, and may be subject to disclosure upon request, pursuant to the requirements of the Wisconsin open records law, sections 19.31 - 19.39 of the Wisconsin Statutes.

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Attn: Bill Bertrand, P.E.

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