



Wisconsin Department of Transportation

February 5, 2015

Division of Transportation Systems Development

Bureau of Project Development
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NOTICE TO ALL CONTRACTORS:

**Proposal #23: 1560-31-71, WISC 2015 097
 Cumberland - Spooner
 Woodyard Rd to CTH B East Junction
 USH 63
 Washburn County**

Letting of February 10, 2015

This is Addendum No. 1, which provides for the following:

Special Provisions

| Revised Special Provisions | |
|----------------------------|--|
| Article No. | Description |
| 3 | Prosecution and Progress |
| 4 | Holiday Work Restrictions |
| 9 | Cooperation with Other Contracts |
| 15 | Traffic and Sequence of Work Operations |
| 37 | Traffic Control |
| 57 | Culvert Pipe for Cattle Pass Sta. 638+10, Item SPV.0090.03 |

| Deleted Special Provisions | |
|----------------------------|-------------|
| Article No. | Description |
| 33 | Topsoil |

Schedule of Items

| Revised Bid Item Quantities | | | | | |
|-----------------------------|--------------------|------|--------------|------------------|----------------|
| Bid Item | Item Description | Unit | Old Quantity | Revised Quantity | Proposal Total |
| 628.6005 | Turbidity Barriers | SY | 565 | 63 | 63 |

Plan Sheets

| Revised Plan Sheets | |
|---------------------|---|
| Plan Sheet | Plan Sheet Title (brief description of changes to sheet) |
| 183 | Quantity change to item Turbidity Barriers from 565 SY to 63 SY. |
| 209 | Note for Sta. 638+10 Culvert Pipe for Cattle Pass changed to match special provision. |

The responsibility for notifying potential subcontractors and suppliers of these changes remains with the prime contractor.

Sincerely,

Mike Coleman

Proposal Development Specialist
Proposal Management Section

ADDENDUM NO. 1

1560-31-71

February 5, 2015

Special Provisions

3. Prosecution and Progress

Remove entire article language and replace with the following:

Begin work within ten calendar days after the engineer issues a written notice to do so.

No work is allowed on Parcel 10, TPP sheet 4.06, until after May 1, 2015.

From Station 625+00 - Station 832+40, place lower layer of HMA pavement within 10 days of the surface being milled.

Add the following to standard spec 108.11:

If the contractor fails to complete the work necessary to reopen USH 63 to traffic within four (4) calendar days, the department will assess the contractor \$13,000 in interim liquidated damages for each calendar day that USH 63 remains closed to through traffic. An entire calendar day will be charged for any period of time within a calendar day that the road remains closed beyond 12:01 AM.

If contract time expires prior to completing all work specified in the contract, additional liquidated damages will be affixed according to standard spec 108.11.

All grading, base aggregate, and asphaltic surface for the shared-use path shall be completed by Friday July 24, 2015 for use during the Shell Lake Lion's Club Triathlon held on Saturday July 25, 2015. The contractor shall arrange and conduct two meetings between the contractor, the department, local officials and Shell Lake Lion's Club representative to discuss the project schedule of operations for vehicular traffic, participants, and pedestrian access during the event. The first meeting shall be one month prior to the event and the second meeting shall be two weeks prior to the event. Shell Lake Lion's Club Contact: Eugene Harrington, President, phone (715) 468-7797.

4. Holiday Work Restrictions

Add the following paragraph to the end of the article:

An exception to non holiday weekends only will be allowed south of CTH D, on Saturdays from 6:00 AM until 10:00 pm.

9. Cooperation with Other Contracts

Traffic Control – Detours

Remove entire section language and replace with the following:

For a maximum time of two weeks during the time period of June 8, 2015 to June 30, 2015, USH 63 will be closed to truck traffic. Contractors for project 1560-31-71 and project 1550-21-71 must coordinate the truck and auto closure times and notify each other of the USH 63 closures a minimum of one week in advance of the truck detour. The truck detour and the auto detour which follows the same route as the truck detour will be the responsibility of the contractor for project 1560-31-71. Northbound USH 63 will be closed to truck traffic from the intersection of USH 63 and STH 48 in Cumberland north to the intersection of USH 63 and USH 53 north of Spooner. Also, southbound

USH 63 will be closed to truck traffic from the intersection of USH 63 and USH 53 north of Spooner south to the intersection of STH 48 and USH 63 in Cumberland. During the two week truck detour, USH 63 will be closed to all through traffic for a maximum of 4 days, Monday through Thursday within one work week, to replace sanitary sewer and water main lines from Station 846+00 - 862+00, and to remove a box culvert at Station 709+08 and replace with a concrete culvert pipe at same location. Also for project 1550-21-71, to allow for polystyrene insulation board removal with excavation below subgrade at Station 484+50 - 487+45 and Station 490+00 - 492+59, and a culvert replacement at Station 473+33.

All other short term detour signage will be the responsibility of each contractor under the contractor's respective contract.

15. Traffic Sequence of Work Operations

Replace paragraph 2 with the following:

When construction operations make it impractical to maintain two-way traffic within the project limits, the contractor may restrict traffic to a one-lane operation. The single lane restriction shall apply to any work performed on USH 63 and all intersecting roads under this contract. No more than two single lane restrictions will be allowed on mainline USH 63 south of CTH D at any one time. Such restrictions will only be allowed during daytime hours and only when work is being prosecuted on the project. Two-way traffic shall be restored at the end of each day.

Replace paragraph 10 with the following:

Stage 1A: USH 63 between Station 625+00 and 833+00 will be flagged for construction operations during construction hours and open to 2-lane, 2-way normal traffic after hours and on weekends, except Saturdays on non holiday weekends from 6:00 AM until 10:00 PM south of CTH D.

Construction during this stage includes the reconstructing of Hilltop Road, CTH D, School Entrance, Pederson Drive, ATV/Snowmobile Trail ('T3'), Church parking lot, and U-Turn Commercial Entrance ('C1'). Construction operations requiring flagging include mill and overlay, shoulder widening, turn lane, pipe cleaning and partial replacements of culvert pipes.

Replace paragraph 12 with the following:

Stage 2: USH 63 traffic shifts west onto the widening constructed in Stage 1B, with traffic separated by flexible tubular markers. The construction zone will be located on the east side/northbound lane of USH 63. Construction operations include constructing the northbound lane of USH 63 and paving all asphaltic lower layers, install storm sewer, construct the ATV/Snowmobile Trail and Shared-Use path, the municipal (beach) parking lot, and all adjacent intersections (Parking Entrances, West Lake Drive and CTH B East). The adjacent intersections are to remain open to traffic for half at a time construction. City utilities and lighting on the RT side of USH 63 will be installed during this stage. A detour between 6th Avenue and 3rd Street will be permitted from Monday at 6:00 AM until Friday at 3:00 PM for a maximum time of two work weeks during the time period of June 8, 2015 – June 30, 2015 for storm sewer and city utility crossings, and for culvert work at Station 709+08. While USH 63 is closed, northbound truck traffic will be detoured on STH 48 from USH 63 in Cumberland to USH 53 northbound in Trego. Southbound truck traffic will be detoured on USH 53 southbound from USH 63 in Trego to STH 48 in Cumberland. STH 70 truck traffic enroute to USH 63 southbound will be detoured on USH 53 southbound from the USH53/STH 70 interchange east of Spooner to STH 48 in Cumberland. Coordinate with the contractor for Project 1550-21-71 regarding the timing of the USH 63 closure.

33. Deleted

37. Traffic Control

Replace entire article language with the following:

Add the following to standard spec 643.3.1:

Cover and/or remove traffic control devices from the traveled way and shoulders of the roadway when not in use. Cover or render lighting devices inoperative when not in use. Provide to the engineer, City of Shell Lake Police Department, Washburn County Sheriff's Department, and the State Patrol District Headquarters responsible for that county with the current telephone number(s) which the contractor or their representative can be contacted at all times in the event a safety hazard develops. Repair, replace or restore the damaged or disturbed traffic control devices within two hours from the time notified or made aware of the damaged or disturbed traffic control devices.

Keep appropriate emergency officials informed of routes to provide emergency services. Do not park or store equipment, vehicles, or construction materials within 30 feet of the edge of the traffic lane of any roadway during non-working hours. Utilize two-way radios, and/or additional flag persons, within lane closure areas and at public road intersections, in order to positively direct, control, and safeguard traffic through the work zone.

All contractor vehicles or equipment operating within the project limits shall be equipped with and have flashing yellow lights operating.

All department owned signs that are removed by the contractor because of interference with construction operations shall, unless otherwise authorized by the engineer, be promptly replaced as directed by the engineer. At no time may stop signs be removed or moved without flag persons present.

Contractor equipment shall remain within DOT right-of-way during construction.

57. Culvert Pipe for Cattle Pass Sta. 638+10, Item SPV.0090.03

Replace entire article language with the following:

A Description

This special provision describes furnishing, installing and pressure grouting a 24-inch flexible pipe liner through the barrel and backfilling the void area between the apron endwalls of the existing 42-inch wide x 72-inch high cattle pass with Select Borrow Grade 2 at Station 638+10 as shown on the plans and as hereinafter provided.

B Materials

Only liners listed on the department's prequalified list are eligible for use. Listing on the department's prequalified list does not exempt the manufacturer from providing letters of certification for properties listed in B.1 on pipe delivered to the project.

B.1 Flexible Pipe Liner

Provide 24-inch flexible pipe liner that conforms to the following requirements:

| | | |
|-----------------------------|------------|---------|
| HDPE | | |
| HDPE | ASTM D3350 | 345463C |
| Profile Wall Pipe Stiffness | ASTM F894 | |
| Solid Wall SDR | ASTM F714 | |

B.2 Pressure Grouting

Provide material consisting of: 1 part (by volume) Portland cement Type I or II and 3 parts (by volume) of sand and water to achieve required fluidity. Provide sand meeting the requirements of 501.2.5. A cellular concrete grout alternative as approved by the engineer may be used.

B.3 Commercial Grout, Cellular Concrete

Provide commercial grout, cellular concrete that conforms to the following requirements:

| | | |
|----------------------|----------------------------|---|
| Cement | ASTM C150 | Type I,II |
| Density | ASTM C495 (no oven drying) | 50 pcf min |
| Compressive Strength | ASTM C495 | 300 psi @ 28 day min 100 psi in 24 hours |
| Shrinkage | ASTM | 1% by volume |
| Flow | ASTM C939 | 35 sec max |

C Construction

C.1 Excavating and Cleaning

Clear the pavement inside the barrel and the apron endwalls of all debris and water, and clean the apron endwalls, pavement and barrel sides of the cattle pass and dry them substantially. Place Select Borrow Grade 2 that consists substantially of sand with all particles retained on a one-inch sieve removed to backfill any voids inside the barrel and apron endwalls to support the HDPE liner and steel apron endwalls at the flow line shown on the cattle pass cross section.

C.2 Plastic Liners

Do not dump from the truck the liners as delivered on the project; instead, unload the liners using slings and boom-type trucks or equivalents. Chains or wire rope will not be permitted for handling.

Connect joints in accordance to the manufacturer's recommendations and as directed by the engineer.

After the liner is in place, fill the area above the cattle pass invert and between the liner and the cattle pass floor and barrel sides, on the top and sides completely with grout. Fill the area above the cattle pass invert and between the liner and the cattle pass endwalls on the sides completely with Select Borrow Grade 2 to the top of the endwalls. Obtain a substantially uniform space between the liner and the cattle pass barrel sides and endwalls.

C.3 Pressure Grouting

Use a grout plant that is capable of accurately measuring, proportioning, mixing, and discharging by volume and at appropriate discharge pressures per the liner manufacturer's specifications. Grout may be placed in lifts to avoid exceeding maximum recommended pressures and shall not exceed the manufacturer's recommendations.

Block, grout in lifts, or secure liners to prevent floatation from buoyant force associated with the grouting operations.

Replace all pipe sections damaged or collapsed from the installation or grouting operations, and for any other associated costs with the replacement operation.

The grout used should be fully contained and should not be allowed to enter any waterways or wetlands.

D Measurement

The department will measure Culvert Pipe for Cattle Pass Sta. 638+10 in length by the linear foot in place for each location acceptably completed.

E Payment

The department will pay for measured quantities at the contract unit price under the following bid item:

| ITEM NUMBER | DESCRIPTION | UNIT |
|-------------|--|------|
| SPV.0090.03 | Culvert Pipe for Cattle Pass Sta. 638+10 | LF |

Payment is full compensation for cleaning the cattle pass; furnishing the 24-inch pipe liner; installing the liner; pressure grouting the entire angular space around the culvert pipe liner within the cattle pass barrel section, including furnishing cement and sand; furnishing, placing and compacting Select Borrow Grade 2 in the void space created by the cattle pass endwalls; performing all excavation and/or pumping needed; obtaining any easements for installing long section of pipe.; properly disposing of waste materials; all materials, labor, tools, equipment and incidentals necessary to complete the contract work.

Steel apron endwalls to be attached to the ends of the HDPE pipe liner, earthwork, landscaping, and erosion control items for slope flattening will be measured and paid for under the respective bid items.

Schedule of Items

Attached, dated February 5, 2015, are the revised Schedule of Items Page 14.

Plan Sheets

The following 8½ x 11-inch sheets are attached and made part of the plans for this proposal:

Revised: 183 and 209

END OF ADDENDUM

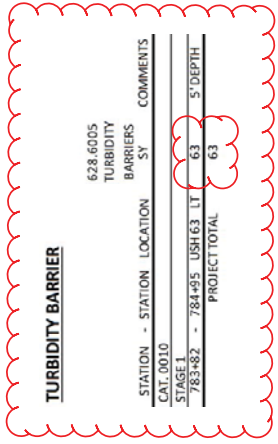
SILT FENCE ITEMS

Table with columns: STATION, LOCATION, LF, SILT FENCE MAINTENANCE, LF. Includes sub-totals for STAGE 1A, STAGE 1B, and PROJECT TOTALS.

EROSION CONTROL MOBILIZATIONS

Table with columns: STATION, LOCATION, EACH, MOBILIZATION CONTROL, EACH, MOBILIZATION EMERGENCY EROSION CONTROL, EACH.

TURBIDITY BARRIER



TEMPORARY DITCH CHECKS

Table with columns: STATION, LOCATION, LF, TEMPORARY DITCH CHECKS, LF. Includes sub-totals for STAGE 1A, STAGE 2, and PROJECT TOTALS.

ROCK BAGS

Table with columns: STATION, LOCATION, EACH, ROCK BAGS, EACH.

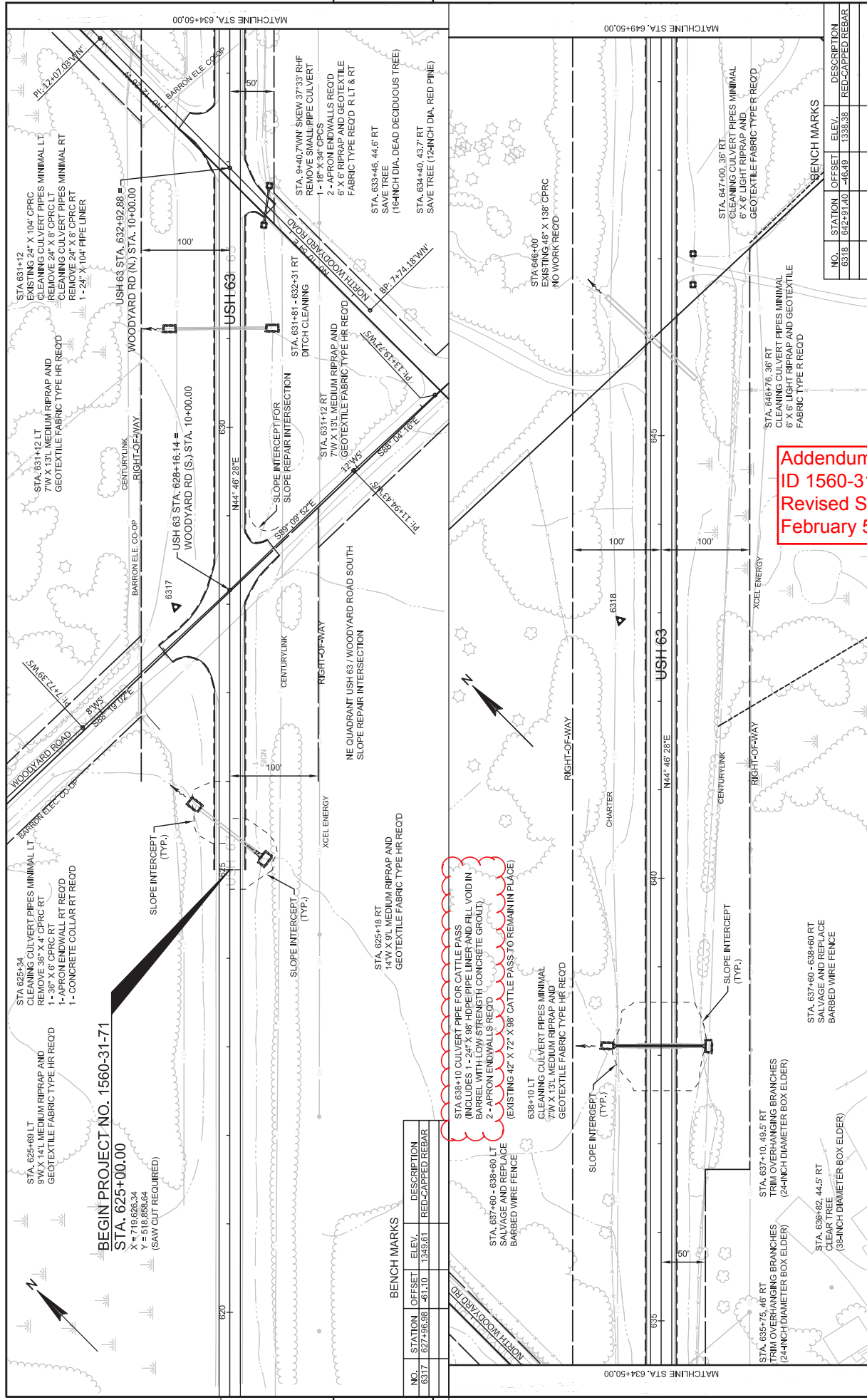
MARKERS CULVERT ENDS

Table with columns: STATION, LOCATION, EACH, MARKERS CULVERT ENDS, EACH.

DELINEATORS FLEXIBLE

Table with columns: STATION, LOCATION, EACH, DELINEATORS FLEXIBLE, EACH.

Addendum No. 1
ID 1560-31-71
Revised Sheet 183
February 5, 2015



BEGIN PROJECT NO. 1560-31-71
 STA. 625+00.00
 X = 719,626.34
 Y = 518,895.64
 (SAW CUT REQUIRED)

| BENCH MARKS | | |
|-------------|-----------|----------------------------|
| NO. | STATION | DESCRIPTION |
| 6317 | 627+95.98 | -81.10 RED-CAPPED REBAR |

STA 638+10 CULVERT PIPE FOR CATTLE PASS
 (INCLUDES 1'-24" X 98" HOPE PIPE LINER AND FILL VOID IN
 BARREL WITH LOW STRENGTH CONCRETE GROUT)
 2 - APRON ENDWALLS REQD
 SALVAGE AND REPLACE
 BARBED WIRE FENCE
 (EXISTING 42" X 72" X 98" CATTLE PASS TO REMAIN IN PLACE)

| BENCH MARKS | | |
|-------------|-----------|----------------------------|
| NO. | STATION | DESCRIPTION |
| 6318 | 642+91.40 | -46.49 RED-CAPPED REBAR |

Addendum No. 1
 ID 1560-31-71
 Revised Sheet 209
 February 5, 2015

SCHEDULE OF ITEMS

REVISED:

CONTRACT:
20150210023PROJECT(S):
1560-31-71FEDERAL ID(S):
WISC 2015097

CONTRACTOR : _____

| LINE NO | ITEM DESCRIPTION | APPROX. QUANTITY AND UNITS | UNIT PRICE | | BID AMOUNT | |
|------------|--|----------------------------------|------------|-----|------------|-----|
| | | | DOLLARS | CTS | DOLLARS | CTS |
| 1410 | 628.1520 Silt Fence Maintenance | 17,073.000 LF | . | . | . | . |
| 1420 | 628.1905 Mobilizations Erosion Control | 15.000 EACH | . | . | . | . |
| 1430 | 628.1910 Mobilizations Emergency Erosion Control | 15.000 EACH | . | . | . | . |
| 1440 | 628.2004 Erosion Mat Class I Type B | 7,493.000 SY | . | . | . | . |
| 1450 | 628.2006 Erosion Mat Urban Class I Type A | 5,514.000 SY | . | . | . | . |
| 1460 | 628.2008 Erosion Mat Urban Class I Type B | 3,083.000 SY | . | . | . | . |
| 1470 | 628.2023 Erosion Mat Class II Type B | 2,292.000 SY | . | . | . | . |
| 1480 | 628.2033 Erosion Mat Class III Type B | 1,063.000 SY | . | . | . | . |
| 1490 | 628.6005 Turbidity Barriers | 63.000 SY | . | . | . | . |
| 1500 | 628.6505 Soil Stabilizer Type A | 0.510 ACRE | . | . | . | . |
| 1510 | 628.6510 Soil Stabilizer Type B | 16.520 ACRE | . | . | . | . |