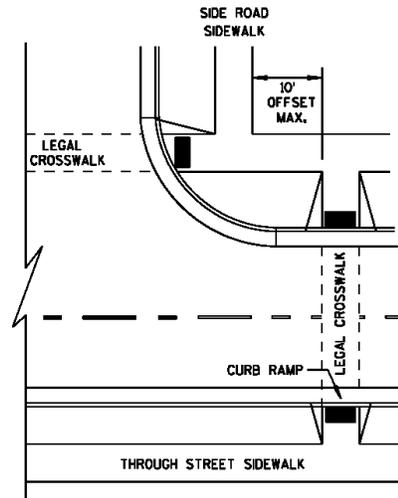
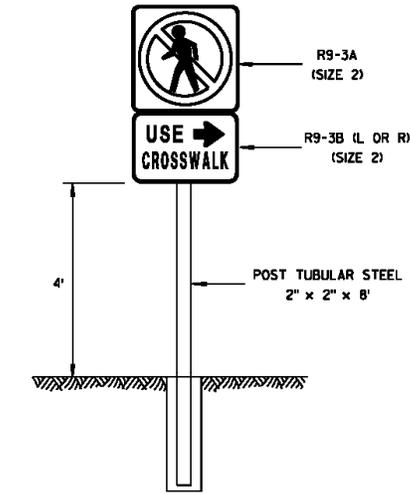


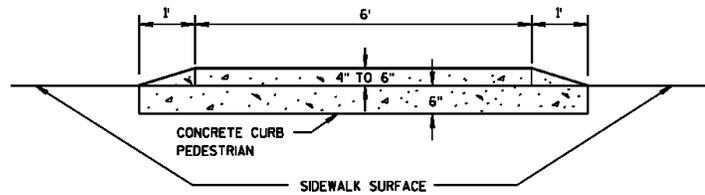
**LEGAL CROSSWALK LAYOUT**  
**FIGURE 1**



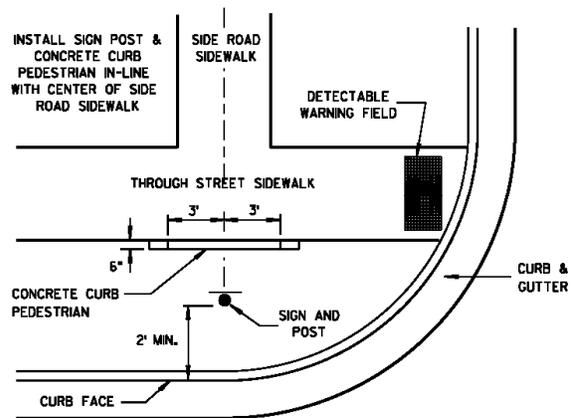
**SIDEWALK TO CROSSWALK OFFSET**  
**FIGURE 2**



**CROSSWALK CLOSURE SIGN**  
**FIGURE 3**



**CONCRETE CURB PEDESTRIAN DETAIL**  
**CURB ELEVATION VIEW**



**CURB AND SIGN LOCATION DETAIL**  
**FIGURE 4**

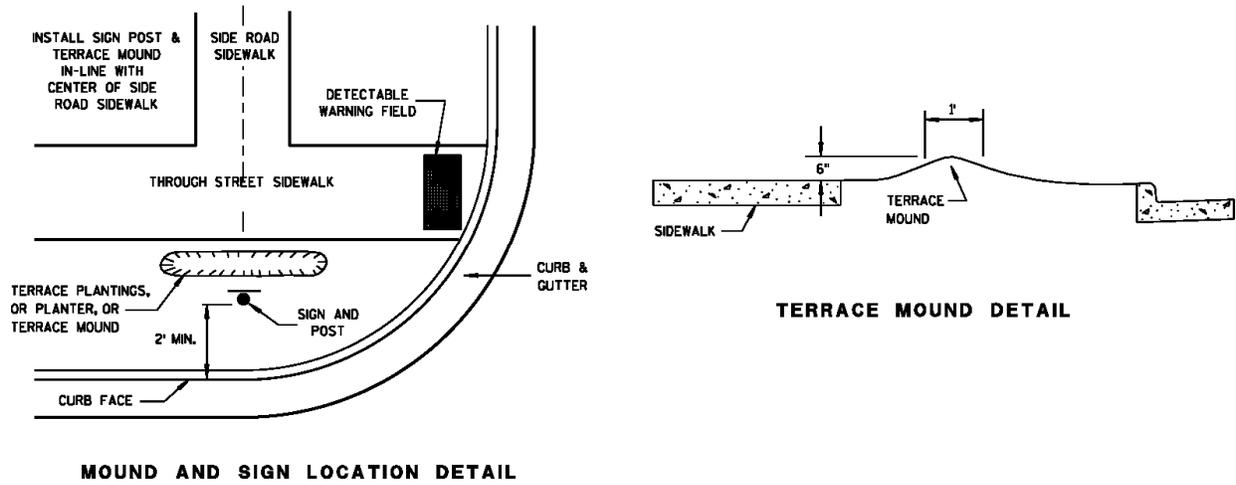


FIGURE 5

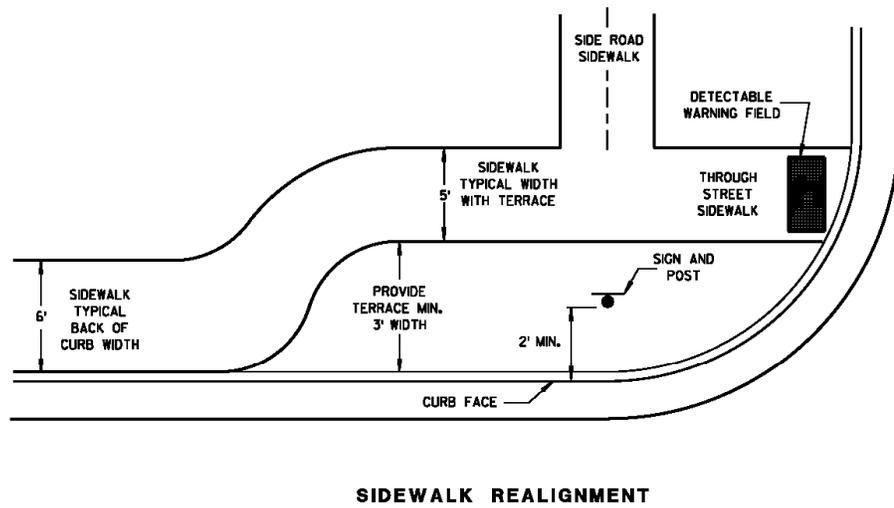
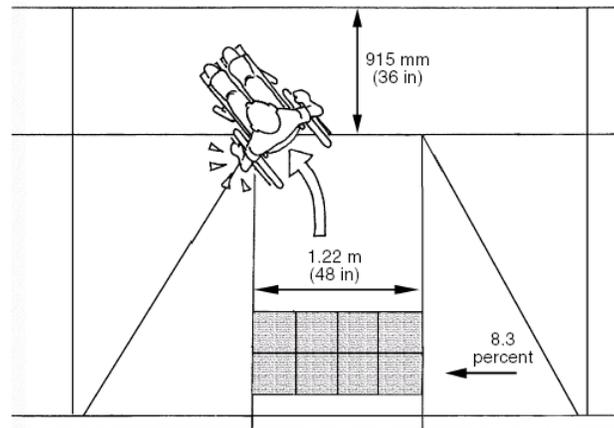
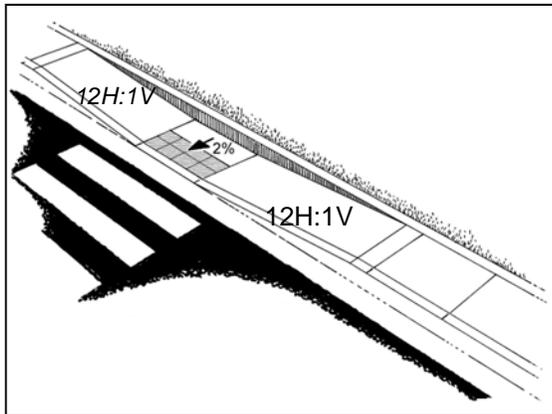
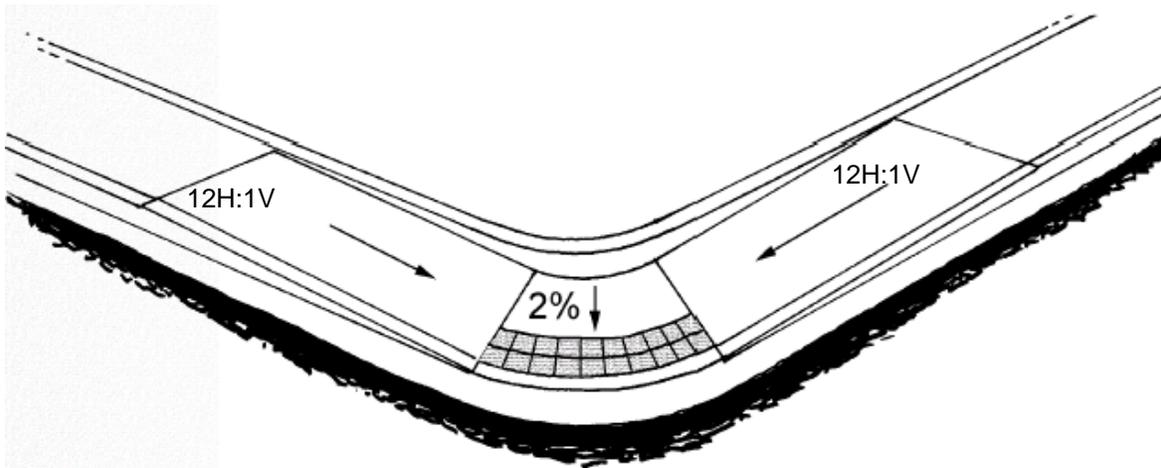
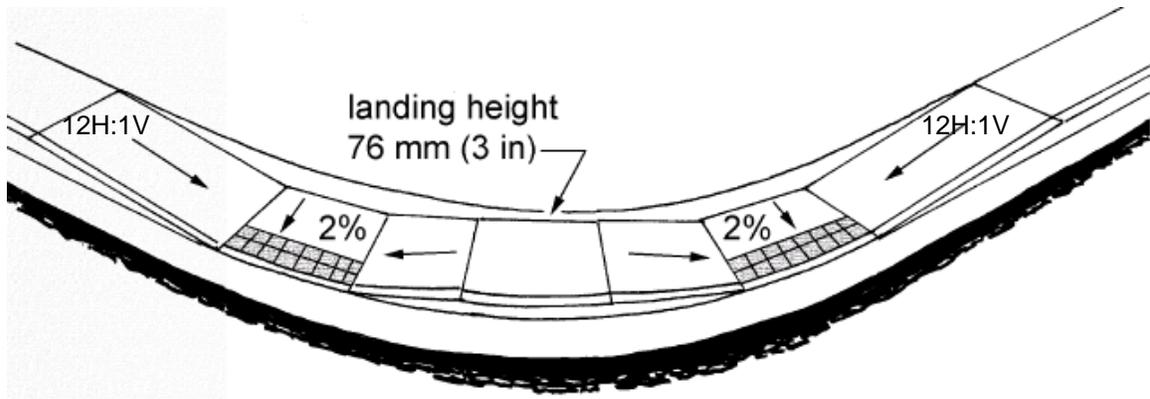


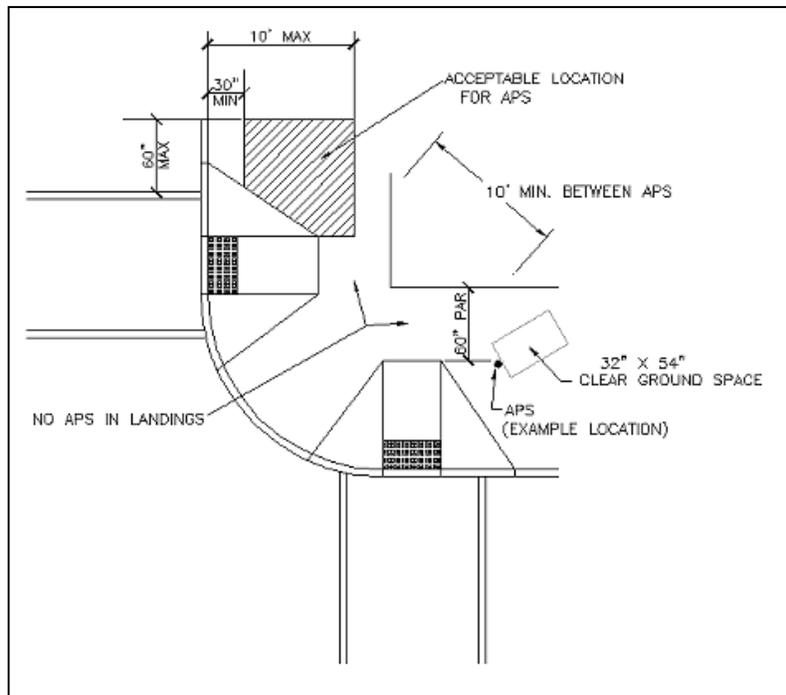
FIGURE 6

**Curb Ramp Options With Restricted Right-of-Way and Mid-block Crossing**



A parallel curb ramp has two ramps leading down, 12H:1V or flatter, towards a center level landing at the bottom between both ramps. The center landing slopes 2 percent toward the back of curb. The side flares slope, 12H:1V (8.3%) or flatter, toward the center landing as well.

## Accessible Pedestrian Signals



### Push Button Location

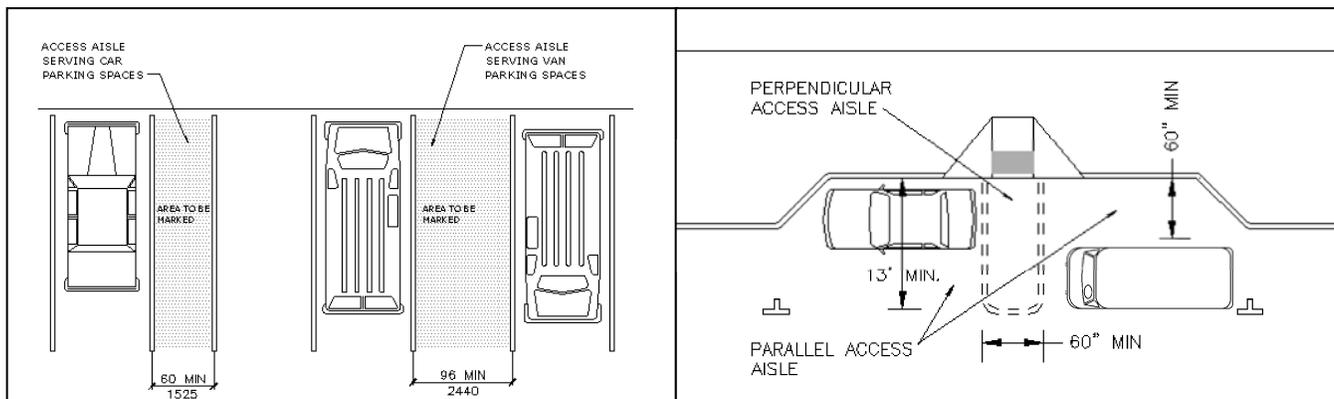
The push button location can have a significant impact on usability for wheelchair users and other people with disabilities. At signalized intersections consider a 2-stage crossing. For this reason, the PROWAAC<sup>1</sup> report established the following requirements for locating push buttons (*U.S. ATBCB<sup>2</sup>, 2001: <http://www.access-board.gov/prowac/commrept/>*).

- Adjacent to landing - locate the push button adjacent to a level landing (maximum slope = 2% in all directions) at least 2.5 ft x 4.5 ft.
- Direction of control face - mount the control face parallel to the direction of the corresponding crosswalk and no closer than 2.5 ft to the curb.
- Mounting height - mount push button 3.5 ft above the sidewalk.
- Close to crosswalk - mount no further than 5 ft from the extension of the crosswalk lines and within 10 ft of the curb line, unless the curb ramp is longer than 10 ft. On median islands mount within 2 ft of the crosswalk and accessible from a wheelchair.
- Proximity to curb ramp - place the push button within 2 ft horizontally of the top corner of the ramp. At a parallel ramp, the push button should be adjacent to the lower landing.
- Separation - where there are two accessible pedestrian signals at the same corner, mount the push buttons on poles separated by at least 10 ft.

<sup>1</sup> Public Rights-of-Way Access Advisory Committee

<sup>2</sup> U.S. Architectural and Transportation Barriers Compliance Board (the Access Board)

### On-Street Parking for People with Mobility Impairments



To address the needs of people with mobility impairments, the PROWAC committee provided guidance on how to address on-street parking. According to their report, if on-street parallel parking is provided in commercial areas, it should include accessible spaces for pedestrians with mobility impairments. The design guidance for the accessible parking spaces can be found in their report (*U.S. ATBCB, 2001*: <http://www.access-board.gov/prowac/commrept/>).

For perpendicular and diagonal parking, the committee further specified that one of every eight spots of mobility-impaired parking be van accessible. If fewer than eight spots of mobility-impaired parking will be provided, one of them shall be van accessible. Van spaces have larger design guidance than other accessible parking spaces (*U.S. ATBCB, 2001*).

In addition to providing accessible parking spaces, designers should also provide an accessible curb ramp that connects the access aisle to the pedestrian zone.