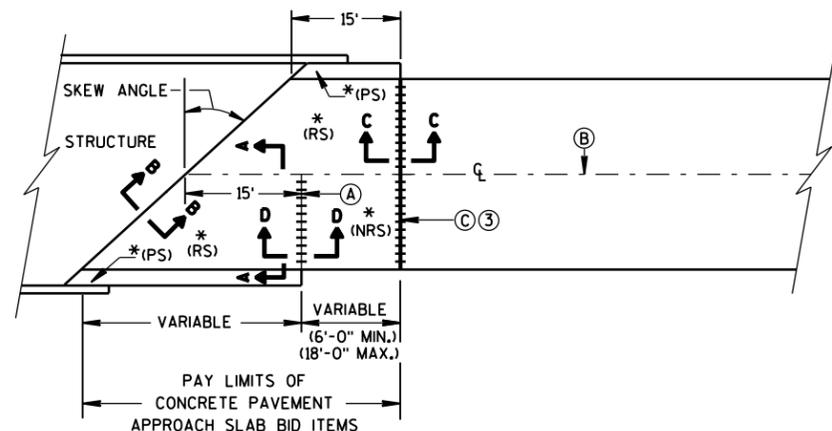
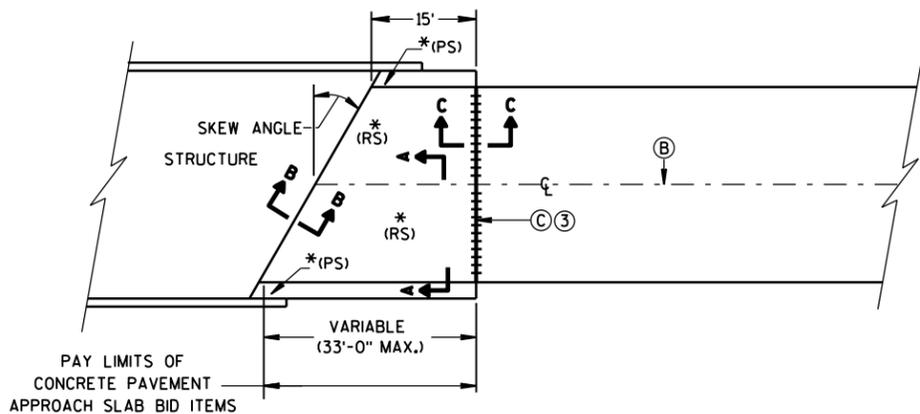


**SKewed APPROACH
(PAVEMENT MORE THAN 2 LANES)**



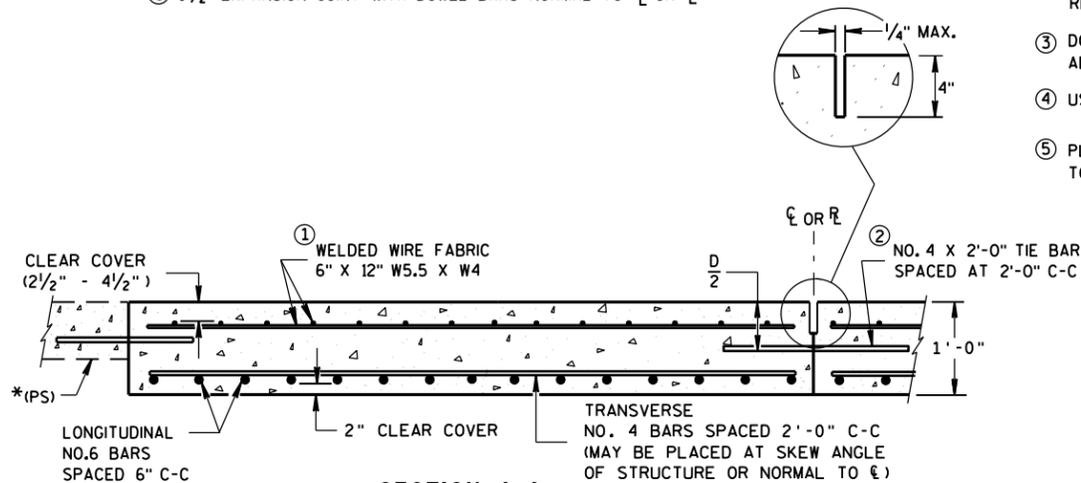
**SKEWS > 20°
(PAVEMENT WIDTH ≤ 30')**



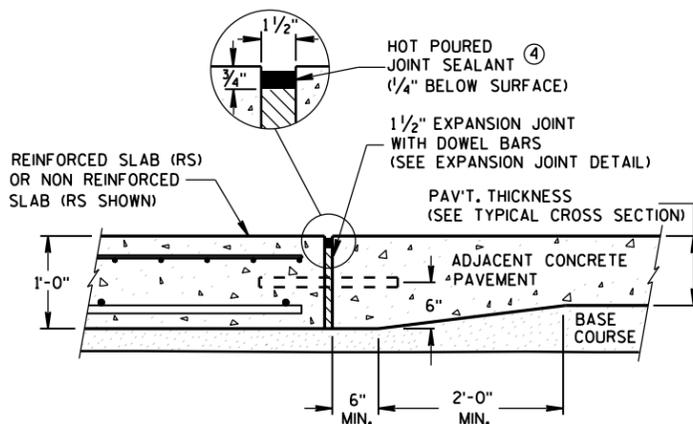
**SKEWS ≤ 20°
(PAVEMENT WIDTH ≤ 30')
APPROACH SLAB AND ADJACENT PAVEMENT**

* (RS) = REINFORCED CONCRETE SLAB
 *(PS) = PAVED CONCRETE SHOULDER OR CONCRETE DRAINAGE SLAB
 (SEE DETAILS ELSEWHERE IN THE PLAN)
 *(NRS) = NON-REINFORCED CONCRETE SLAB
 *** STANDARD DOWEL BAR DIAMETER
 (SEE SDD 13C11, & SDD 13C13)

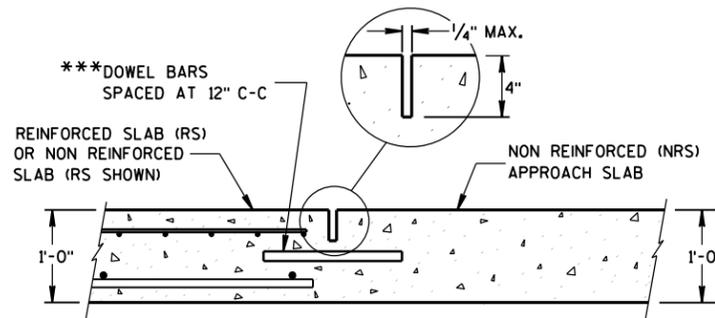
- (A) STANDARD CONTRACTION JOINT NORMAL TO ℓ OR ℓ_c
- (B) STANDARD LONGITUDINAL JOINT WITH TIE BARS.
- (C) 1 1/2" EXPANSION JOINT WITH DOWEL BARS NORMAL TO ℓ OR ℓ_c



**SECTION A-A
REINFORCEMENT POSITIONING DETAIL**



**SECTION C-C
TRANSITION DETAIL
APPROACH SLAB TO ADJACENT PAVEMENT**



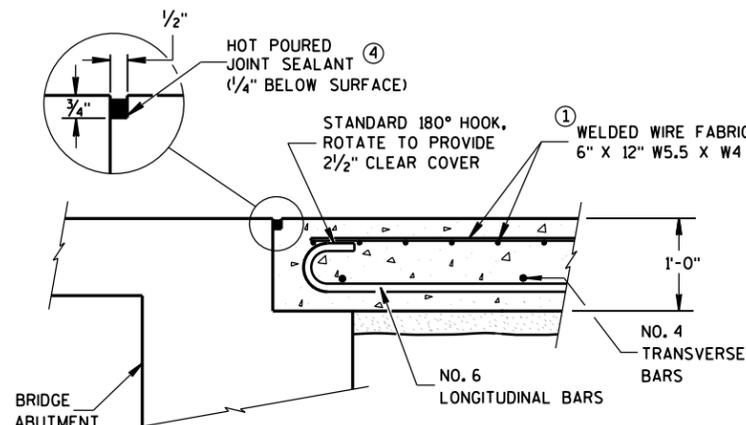
**SECTION D-D
CONTRACTION JOINT**

GENERAL NOTES

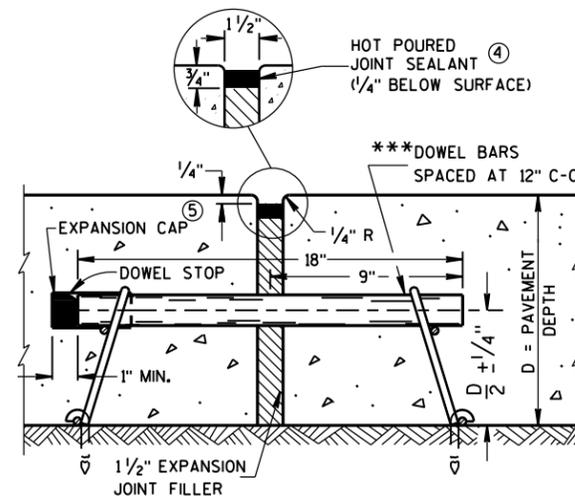
THE CONTRACTOR MAY SPLICE NO. 6 BARS IN THE APPROACH SLAB FOR SKEWED STRUCTURES ONLY. STAGGER SPLICES WITH A MAXIMUM OF ONE SPLICE PER BAR. THE LENGTH OF LAP IS 20 INCHES.

TACK WELD DOWEL BARS TO THE BASKETS ON ALTERNATE ENDS.

- ① THE CONTRACTOR MAY USE NO. 4 BARS SPACED AT 2'-0" C-C IN BOTH THE LONGITUDINAL AND TRANSVERSE DIRECTIONS FOR TOP REINFORCEMENT AS AN ALTERNATIVE TO THE WELDED WIRE FABRIC.
- ② THE CONTRACTOR MAY OMIT TIE BARS BETWEEN REINFORCED SLABS WHERE SLAB REINFORCEMENT BARS EXTEND ACROSS THE CENTERLINE OR REFERENCE LINE.
- ③ DO NOT CONSTRUCT AN EXPANSION JOINT OR INSTALL DOWEL BARS WHEN ABUTTING AN HMA PAVEMENT.
- ④ USE A JOINT SEALANT MEETING THE REQUIREMENTS OF ASTM D6690.
- ⑤ PLACE EXPANSION CAP ON THE END OF THE DOWEL THAT IS NOT TACK WELDED TO THE BASKET. DO NOT FORCE DOWEL BAR PAST THE DOWEL STOP.



**SECTION B-B
BEND DETAIL
BOTTOM REINFORCEMENT**



EXPANSION JOINT DETAIL

**CONCRETE PAVEMENT
APPROACH SLAB**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
June, 2015 /S/ Peter Kemp, P.E.
DATE PAVEMENT SUPERVISOR
FHWA

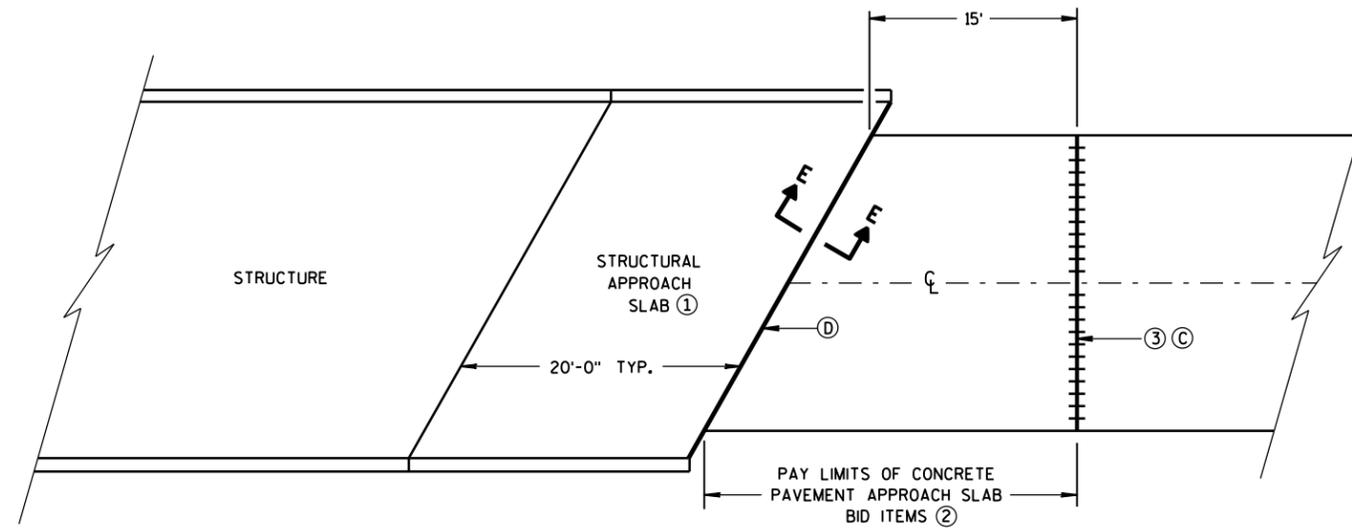
GENERAL NOTES

ALL PROJECTS THAT INVOLVE A STRUCTURAL APPROACH SLAB WILL ALSO HAVE A CONCRETE PAVEMENT APPROACH SLAB.

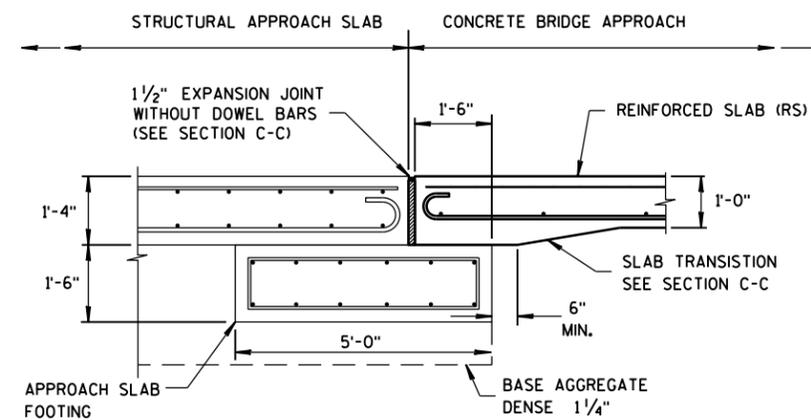
- ① SEE BRIDGE PLAN.
- ② CONFORM TO SHEET 13 B 2(A) FOR CONCRETE PAVEMENT APPROACH SLAB DETAILS.
- ③ DO NOT CONSTRUCT AN EXPANSION JOINT OR INSTALL DOWEL BARS WHEN ABUTTING AN HMA PAVEMENT.

Ⓒ 1½" EXPANSION JOINT WITH DOWEL BARS NORMAL TO \bar{r}_L OR \bar{r}_C

Ⓓ 1½" EXPANSION JOINT (NO DOWELS)



BRIDGE APPROACHES



**SECTION E-E
FOOTING DETAIL**

STRUCTURAL APPROACH SLAB TO CONCRETE BRIDGE APPROACH

**STRUCTURAL APPROACH SLAB
AND CONCRETE PAVEMENT
APPROACH SLAB**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
June, 2015 DATE /S/ Peter Kemp, P.E. PAVEMENT SUPERVISOR
FHWA

*Concrete Pavement Approach Slab***References:**[FDM 14-10-15](#)**Bid items associated with this drawing:**

<u>ITEM NUMBER</u>	<u>DESCRIPTION</u>	<u>UNIT</u>
415.0060-0199	Concrete Pavement (Inch)	SY
415.1080-1199	Concrete Pavement HES (Inch)	SY
415.0410	Concrete Pavement Approach Slab	SY
415.1410	Concrete Pavement Approach Slab HES	SY

NOTE: Bid items associated with Structural Approach Slab and Approach Slab Footing will be included in the bridge plan.

Standardized Special Provisions associated with this drawing:

<u>STSP NUMBER</u>	<u>TITLE</u>
NONE	

Other SDDs associated with this drawing:

Bureau of Structures - LRFD Standard Details (Chapter 12 - Abutments
<http://wisconsindot.gov/dtsdManuals/strct/manuals/bridge/ch12.pdf>)

Design Notes:

[FDM 14-10-15](#) provides guidance on bridge approach pavements. This SDD reflects AASHTO's recommendation of providing pavement expansion away from the bridge abutment.

A *Structural Approach Slab* and a *Concrete Bridge Approach* is required on all I.H. and U.S.H. bridges.

Use Sheets A and B together in plan sets for all projects involving Structural Approach Slabs. Consider Structural Approach Slabs for other locations. Coordinate directly with the chief structural design engineer for any questions regarding Structural Approach Slabs or Approach Slab Footings.

Only Sheet A is needed for projects involving Concrete Bridge Approach. Pay limits for the Concrete Pavement Approach Slab bid items are shown in the drawings. The remaining area of the Concrete Bridge Approach, including the additional expansion joints, is paid for under the Concrete Pavement bid items.

Bid items associated with Structural Approach Slab and Approach Slab Footing will be included in the bridge plan.

Contact Person:

Myungook Kang (MK) (608) 246-7957