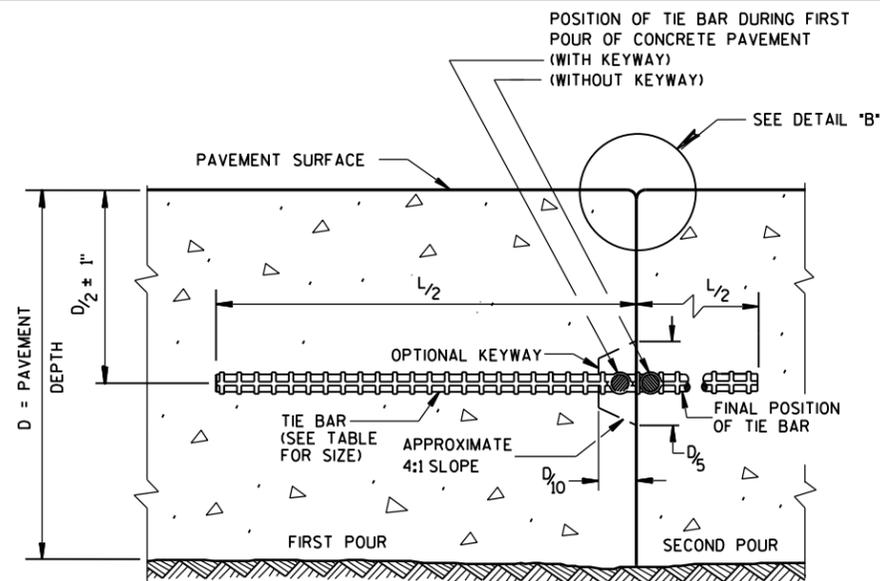
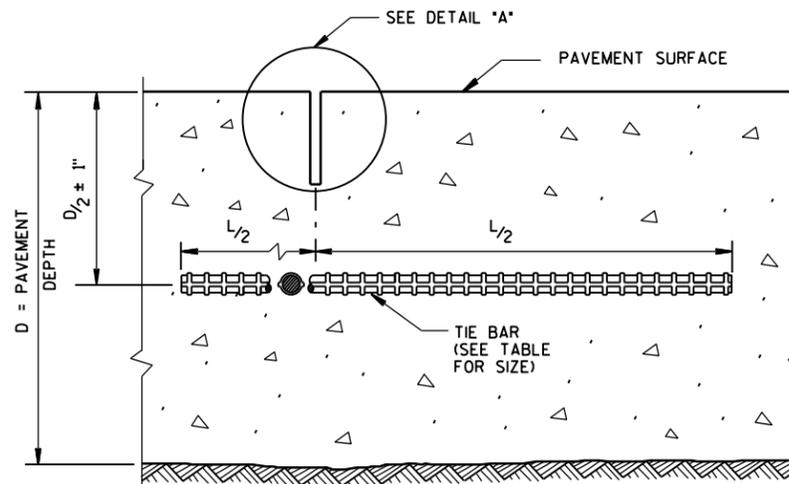




13C1: Concrete Pavement Longitudinal Joints and Ties



CONSTRUCTION JOINT

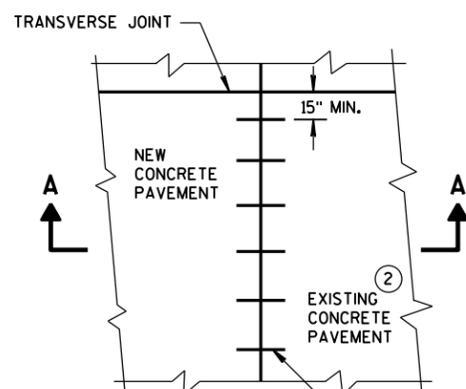


SAWED JOINT

GENERAL NOTES

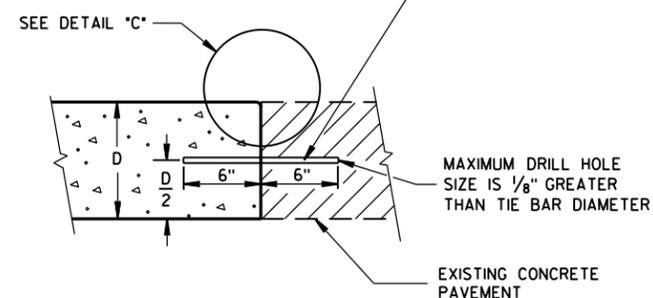
- DO NOT SEAL OR FILL LONGITUDINAL JOINTS.
- CREATE A LONGITUDINAL JOINT FOR PAVEMENT WIDTHS GREATER THAN 15 FEET.
- CORRELATE LONGITUDINAL JOINTS WITH LANE LINES WHEN POSSIBLE.

- ① ANCHOR TIE BARS INTO DRILLED HOLES WITH AN EPOXY.
- ② PAVEMENT THAT WAS IN PLACE PRIOR TO THE CONTRACT.

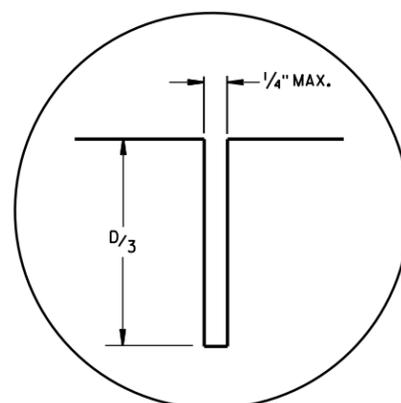


PLAN VIEW

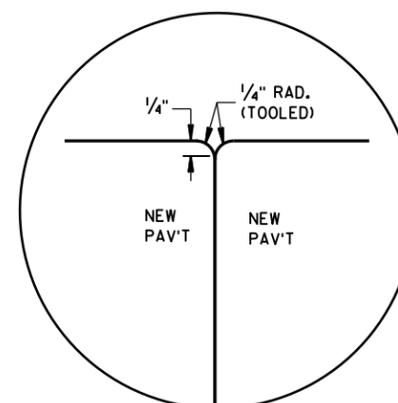
NO. 6 TIE BARS SPACED 30" C-C, INSTALLED PERPENDICULAR TO THE LONGITUDINAL JOINT. ①



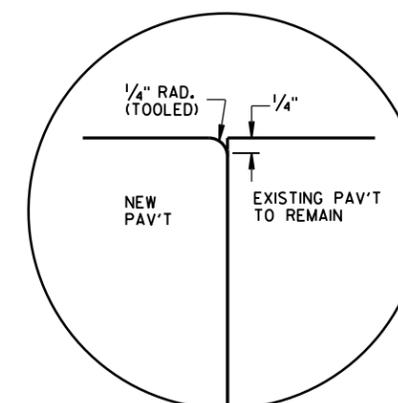
**SECTION A-A
LONGITUDINAL CONSTRUCTION JOINT
TIE BARS ANCHORED
INTO EXISTING PAVEMENT**



DETAIL "A"



DETAIL "B"



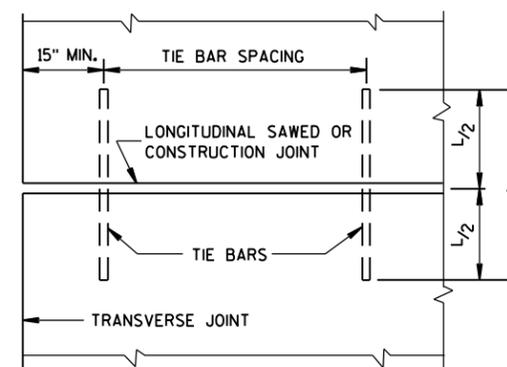
DETAIL "C"

TIE BAR TABLE

PAVEMENT DEPTH (D)	TIE BAR SIZE	TIE BAR LENGTH (L)	MAX. TIE BAR SPACING
< 10 1/2"	NO. 4	30"	36"
≥ 10 1/2"	NO. 5	36"	36"
	NO. 4 *	30"	24" **

* SUBSTITUTE BENT BARS AT LONGITUDINAL JOINTS WHEN EQUIPMENT LIMITATIONS DURING CONSTRUCTION WARRANT (e.g. AUXILIARY LANES OR TURN LANES)

** CONFORM TO 15" MINIMUM SPACING FROM TRANSVERSE JOINTS; SPACING BETWEEN TIE BARS WILL BE 30" AT TRANSVERSE JOINTS.



**PLAN VIEW
SHOWING LOCATION OF TIE BARS**

CONCRETE PAVEMENT LONGITUDINAL JOINTS AND TIES	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED June, 2015 DATE	/s/ Peter Kemp, P.E. PAVEMENT SUPERVISOR
FHWA	

*Concrete Pavement Longitudinal Joints and Ties***References:**[FDM 14-10-10](#)**Bid items associated with this drawing:**

<u>ITEM NUMBER</u>	<u>DESCRIPTION</u>	<u>UNIT</u>
415.0060 - 0199	Concrete Pavement (inch)	SY
415.1080 - 1199	Concrete Pavement HES (inch)	SY
416.0610	Drilled Tie Bars.....	EACH

Standardized Special Provisions associated with this drawing:

<u>STSP NUMBER</u>	<u>TITLE</u>
NONE	

Other SDDs associated with this drawing:

SDD 13c4	Urban Non-Doweled Concrete Pavement
SDD 13c11	Rural Doweled Concrete Pavement , sheets "a" and "b"
SDD 13c13	Urban Doweled Concrete Pavement

Design Notes:

NONE

Contact Person:

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