










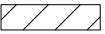
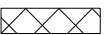
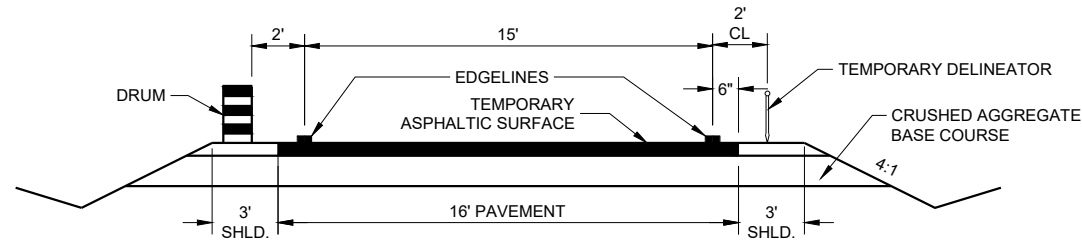


LEGEND

-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
-  DELINEATOR FLEXIBLE / TUBULAR MARKER
-  TEMPORARY DELINEATOR - WHITE (STEEL POST WITH SINGLE DELINEATOR)
-  TYPE "A" WARNING LIGHT (FLASHING)
-  TYPE III BARRICADE
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  FLAGS, 16" X 16" MIN., ORANGE
-  DIRECTION OF TRAFFIC
-  REMOVE PAVEMENT MARKINGS
-  WORK AREA
-  TEMPORARY CROSSOVER ASPHALTIC PAVEMENT



TYPICAL RAMP CROSSOVER ROADWAY DIMENSIONS

** ADJUST RAMP SPEED AS NEEDED IN THE FIELD. DISTANCE TO SIGN LOCATION SHALL ALSO BE PLACED ACCORDING TO TABLE.

** TABLE FOR ADVANCE WARNING SIGN DISTANCE (FT)

POSTED SPEED LIMIT (MPH)	DESIRED EXIT RAMP SPEED (MPH)					
	10	20	30	40	50	60
45	350	300	250			
50	425	400	325	225		
55	500	475	400	300		
60	575	550	500	400	300	
65	650	625	575	500	375	100
70	650	625	575	500	375	375

GENERAL NOTES

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON COVERED OR "DOWNED" SIGNS.

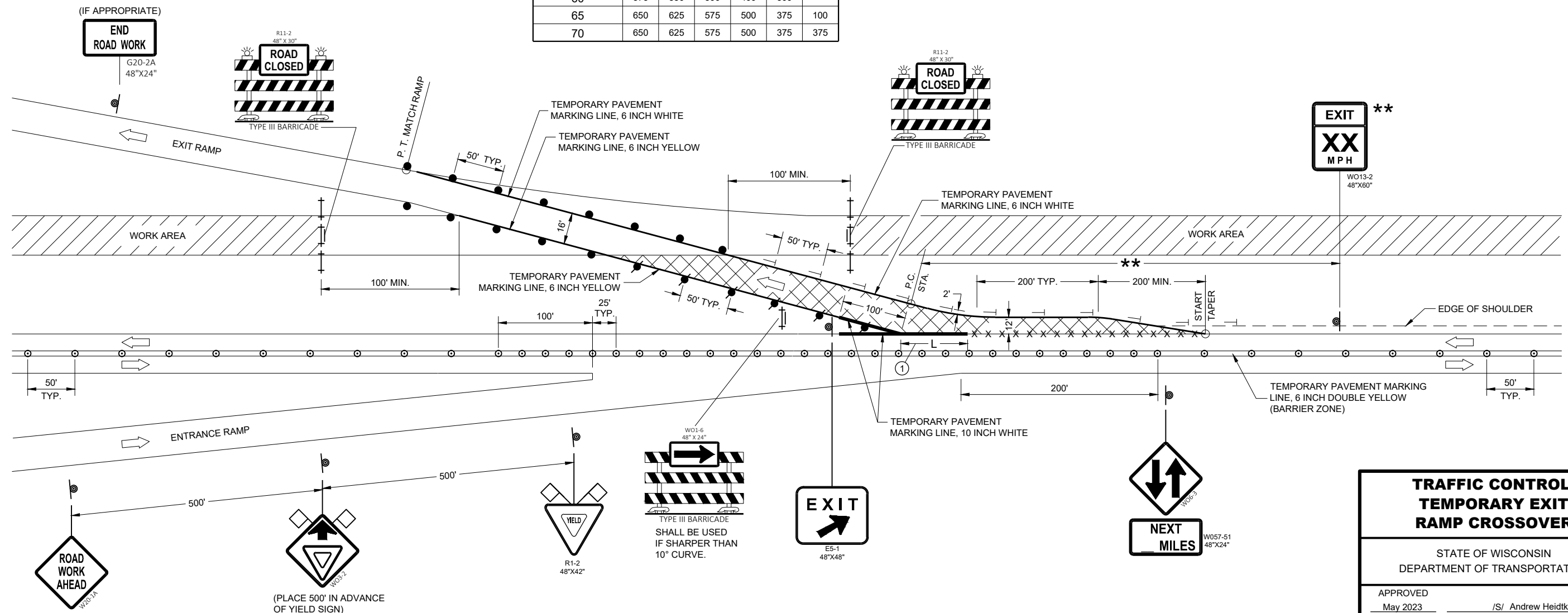
THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) DISTANCE TO EXISTING SIGNS.

WORK ALONG EDGES OF RAMP SHALL MAINTAIN A 10 FOOT AREA CLEAR OF PHYSICAL OBSTRUCTIONS SUCH AS DROP OFFS FROM PAVEMENT REMOVAL OR BROKEN CONCRETE CHUNKS DURING PAVEMENT REMOVAL. THESE AREAS SHALL BE LEVELED WITH TEMPORARY FILL TO PROTECT THE TRAVELING PUBLIC WHEN THE CONTRACTOR IS NOT WORKING ADJACENT TO THE CROSSOVER.

TRAFFIC CONTROL FOR RAMP CROSSOVERS ARE TYPICAL FOR ALL CONSTRUCTION PHASES.

① LENGTH "L" SHALL BE REQUIRED WHEN TANGENT LENGTH OF EXIT RAMP PARALLEL TO MAINLINE EXCEEDS 300 FEET. LENGTH SHALL BE EQUAL TO THE EXIT RAMP TANGENT LENGTH MINUS 300 FEET OR AS APPROVED BY THE ENGINEER AND SHALL CHANGE TO ACCOMMODATE PAVING GAPS.



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SDD 15D07-06

SDD 15D07-06

**TRAFFIC CONTROL,
TEMPORARY EXIT
RAMP CROSSOVER**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED _____ /S/ Andrew Heidtke
DATE _____ WORK ZONE ENGINEER

FHWA

Traffic Control, Temporary Exit Ramp Crossover

References:

Part VI from the Manual on Uniform Traffic Control Devices

[FDM 11-50-20](#)

[FDM 11-10-5](#) Figure 1 and Figure 7

Bid items associated with the drawing:

<u>ITEM NUMBER</u>	<u>Description</u>	
633.1100	Delineators Temporary.....	EACH
643.0300	Traffic Control Drums	DAY
643.0420	Traffic Control Barricades Type III	DAY
643.0500	Traffic Control Flexible Tubular Marker Posts	EACH
643.0600	Traffic Control Flexible Tubular Marker Bases	EACH
643.0705	Traffic Control Warning Lights Type A	DAY
643.0715	Traffic Control Warning Lights Type C	DAY
643.0900	Traffic Control Signs	DAY
643.0910	Traffic Control Covering Signs Type I.....	EACH
643.0920	Traffic Control Covering Signs Type II	EACH
643.3200-3299	Temporary Marking Line (Material) 6-Inch.....	LF
643.3400-3499	Temporary Marking Line (Material) 10-Inch.....	LF
646.1000-01099	Marking Line (Material) 4-Inch.....	LF
646.2000-2099	Marking Line (Material) 6-Inch.....	LF
646.3000-3099	Marking Line (Material) 8-Inch.....	LF
646.4000-4099	Marking Line (Material) 10-Inch.....	LF
646.9000	Marking Removal Line 4-Inch.....	LF
646.9060	Marking Removal Line 6-Inch.....	LF
646.9100	Marking Removal Line 8-Inch.....	LF
646.9160	Marking Removal Line 10-Inch.....	LF

Standardized Special Provisions associated with this drawing:

<u>STSP NUMBER</u>	<u>TITLE</u>
NONE	

Other SDDs associated with this drawing:

SDD 15A4	Delineator and Delineator Post
SDD 15C11	Channelizing Devices

Design Notes:

Analyze drainage for possible need of temporary pipe. Include the pipe quantity as well as necessary erosion control in misc. quantities. Consider geometrics of interchange to avoid obstacles such as piers and inlets.

Advance guide signs providing information related to the temporary exit should be relocated or duplicated adjacent to the temporary roadway. If the signs are duplicated, the temporary signs need to be shown on a traffic control overview sheet. If the guide sign is still visible from the temporary roadway, an "Exit Open" sign may be added near the temporary roadway.

The actual cross-section and type of pavement for the crossover needs to be specified elsewhere on the plan. Typically, the ramp will need two locations to provide suitable paving gaps.

At some locations, a stop condition rather than a yield condition may be required.

Cover existing and permanent signs to prevent conflicts with detours or other traffic operations.

In lieu of covering signs, the contractor may choose to remove and reinstall them.

Contact Person:

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