

GENERAL NOTES

WHEN CLOSING OR RELOCATING CROSSWALKS OR SIDEWALKS, PROVIDE DETECTABLE TEMPORARY FACILITIES AND INCLUDE ACCESSIBILITY FEATURES CONSISTENT WITH EXISTING PEDESTRIAN FACILITIES.

TEMPORARY TRAFFIC CONTROL DEVICES FOR PEDESTRIANS ARE SHOWN. OTHER DEVICES MAY BE NECESSARY TO CONTROL VEHICULAR TRAFFIC. STAGE WORK, AS NECESSARY, TO PROVIDE A TEMPORARY PEDESTRIAN ACCESS ROUTE AT ALL TIMES. FOR ROADWAYS WITH NO AVAILABLE DETOURS, MAINTAIN ONE OPEN SIDEWALK AT ALL TIMES.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

FOR NIGHTTIME CLOSURE USE TYPE "A" FLASHING WARNING LIGHTS ON BARRICADES, SUPPORTING SIGNS AND CLOSING SIDEWALK. USE TYPE "C" STEADY BURN LIGHTS ON CHANNELIZING DEVICES SEPARATING THE WORK AREA FROM VEHICULAR TRAFFIC.

PEDESTRIAN TRAFFIC SIGNAL DISPLAY CONTROLLING CLOSED CROSSWALK SHALL BE COVERED OR DEACTIVATED.

POST MOUNTED SIGNS LOCATED ADJACENT TO A SIDEWALK SHALL HAVE A 7 FOOT MINIMUM CLEARANCE FROM THE BOTTOM OF THE SIGN TO THE SIDEWALK SURFACE.

ALTERNATE SIDEWALK WORK BETWEEN LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.

LEGEND

	SIGN ON PERMANENT SUPPORT		DIRECTION OF TRAFFIC
	UNDER PEDESTRIAN TRAFFIC		TRAFFIC CONTROL DRUM
	WORK AREA		
	PEDESTRIAN CHANNELIZING DEVICE		
	TYPE II BARRICADE WITH/WITHOUT SIGN (ALL WITH ONE WARNING LIGHT, TYPE A, LOW-INTENSITY FLASHING)		
	TYPE III BARRICADE WITH/WITHOUT SIGN (ALL WITH ONE WARNING LIGHT, TYPE A, LOW-INTENSITY FLASHING)		

1 IF SIDEWALK CLOSURE AFFECTS AN ACCESSIBLE AND DETECTABLE FACILITY, MAINTAIN ACCESSIBILITY AND DETECTABILITY ALONG THE ALTERNATE PEDESTRIAN ROUTE.

2 "ROAD WORK AHEAD" SIGNS ARE NOT REQUIRED IF THE SIDEWALK CLOSURE OCCURS WITHIN A LARGER WORK ZONE WHERE ADVANCE WARNING SIGNS ARE ALREADY PRESENT, OR IF THE WORK AREA AND EQUIPMENT ARE MORE THAN 2 FEET BEHIND THE CURB.

3 IF TEMPORARY PEDESTRIAN CROSSWALK IS NOT PROVIDED, OMIT R9-8 AND W011-2 SIGN ASSEMBLIES. IF PROVIDED INCLUDE ON BOTH SIDES OF THE CROSSWALK.

4 TEMPORARY CURB RAMPS. SEE SDD 15 D 30 SHEET "B".

5 DRUMS OR BARRICADES AT 25 FOOT SPACING. STREET PARKING SHALL BE PROHIBITED FOR AT LEAST 50 FEET IN ADVANCE OF THE MID-BLOCK CROSSWALK.

6 TEMPORARY PAVEMENT MARKING FOR CROSSWALK LINES.

7 LIMIT WORK TO ONE QUADRANT AT A TIME TO MINIMIZE PEDESTRIAN DISRUPTION.

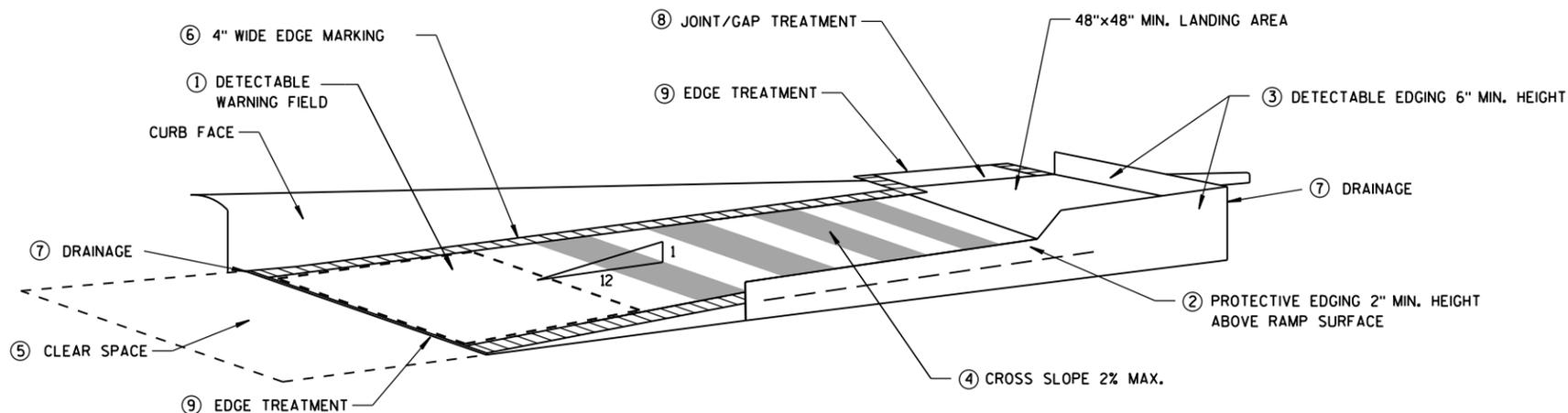
TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

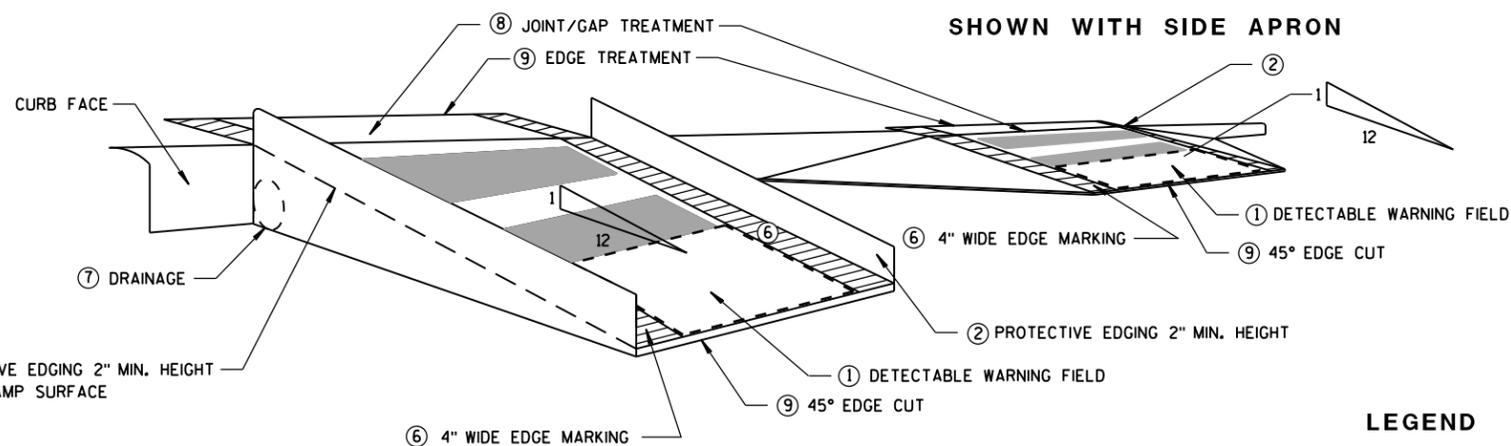


GENERAL NOTES

- NOTIFY THE BUS COMPANY 7 DAYS IN ADVANCE OF THE BUS STOP RELOCATION.
ALTERNATE SIDEWALK WORK BETWEEN LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.
- ① CURB RAMPS SHALL BE 48" MIN. WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE. INSTALL CONTRASTING DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS. REFER TO SDD 8D5 SHEET "E".
 - ② PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
 - ③ DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
 - ④ CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.
 - ⑤ CLEAR SPACE OF 48"x48" MIN. SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.
 - ⑥ THE CURB RAMP WALKWAY EDGE SHALL BE MARKED WITH A YELLOW COLOR, 4" WIDE MARKING, UNLESS A CONTRASTING DETECTABLE WARNING FIELD IS PROVIDED.
 - ⑦ DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
 - ⑧ LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.
 - ⑨ CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES SHALL BE VERTICAL UP TO 1/4" HIGH, AND BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".
 - ⑩ 5' WIDE MIN. WITH PEDESTRIAN SAFETY FENCE, 10' WIDE MIN. WITHOUT PEDESTRIAN SAFETY FENCE.

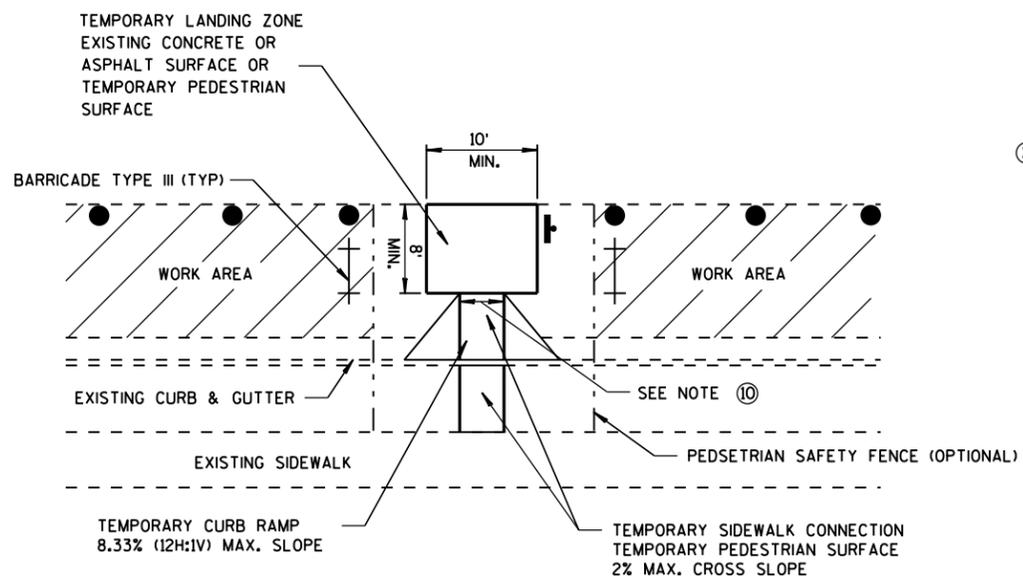


**TEMPORARY CURB RAMP
PARALLEL TO CURB**



SHOWN WITH PROTECTIVE EDGE

**TEMPORARY CURB RAMP
PERPENDICULAR TO CURB**



TEMPORARY BUS STOP PAD

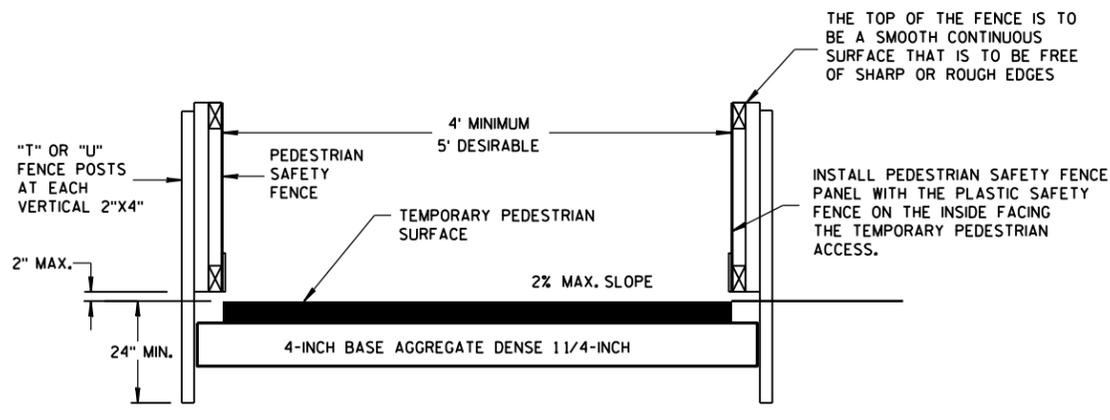
LEGEND

- WORK AREA
- TYPE III BARRICADE
- TRAFFIC CONTROL DRUM

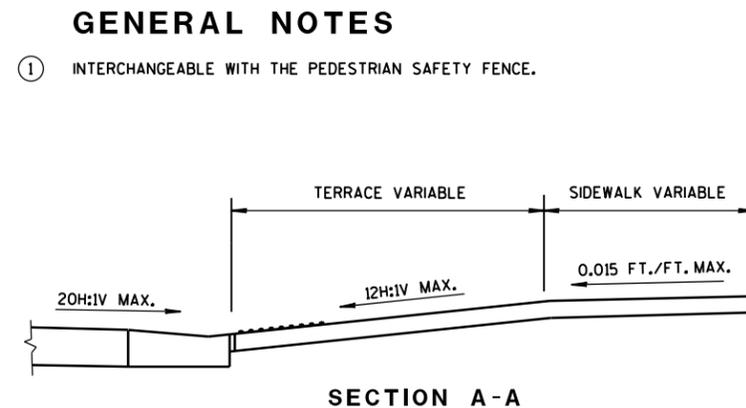
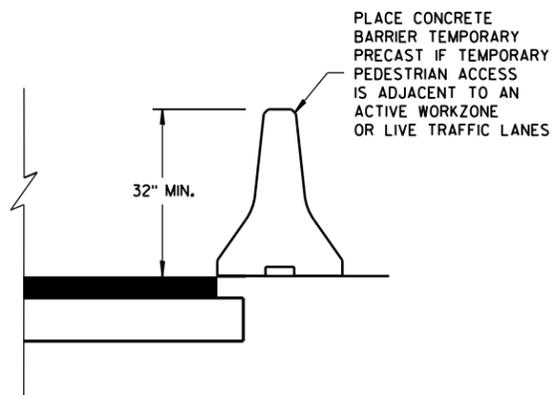
**TRAFFIC CONTROL,
TEMPORARY ADA COMPLIANT
PEDESTRIAN ACCOMMODATION**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
June 2016 /S/ Peter Amakobe Atepe
DATE STATEWIDE WORK ZONE TRAFFIC
FHWA SAFETY ENGINEER

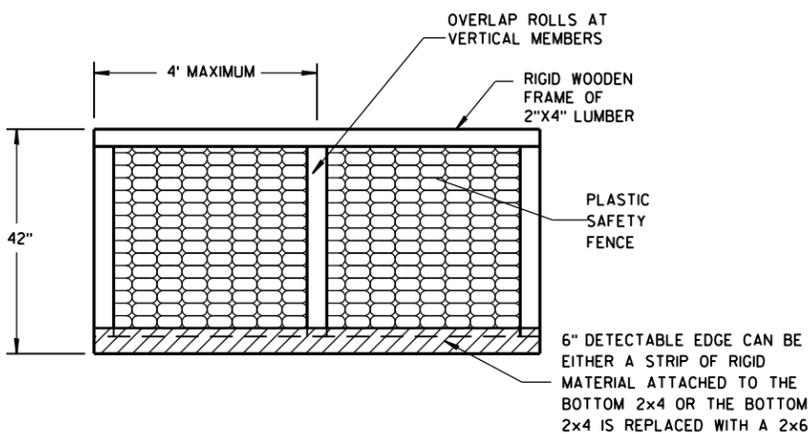


TEMPORARY PEDESTRIAN ACCESS

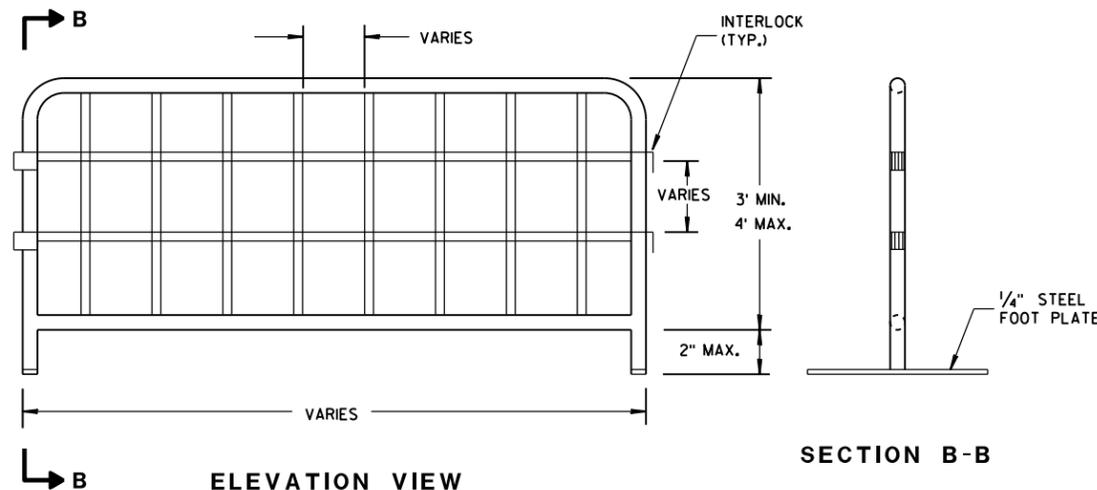


GENERAL NOTES

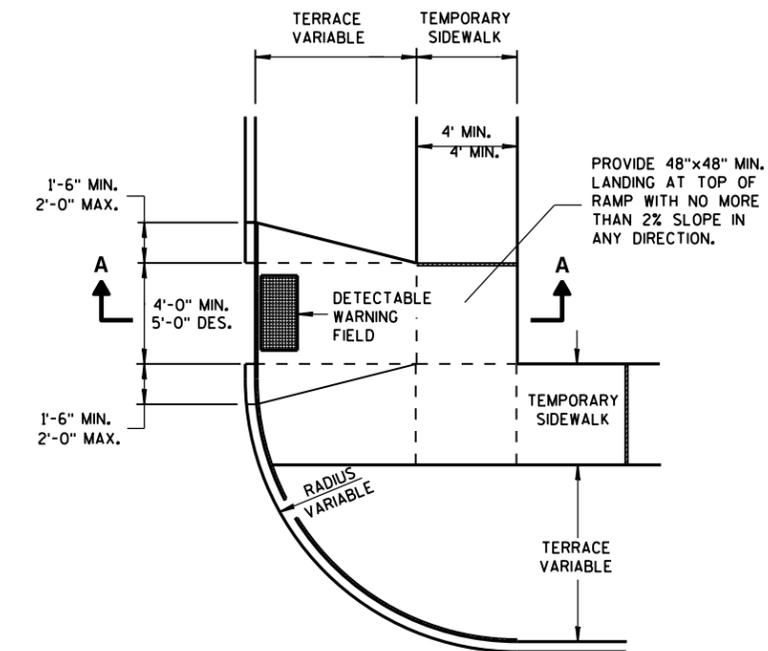
① INTERCHANGEABLE WITH THE PEDESTRIAN SAFETY FENCE.



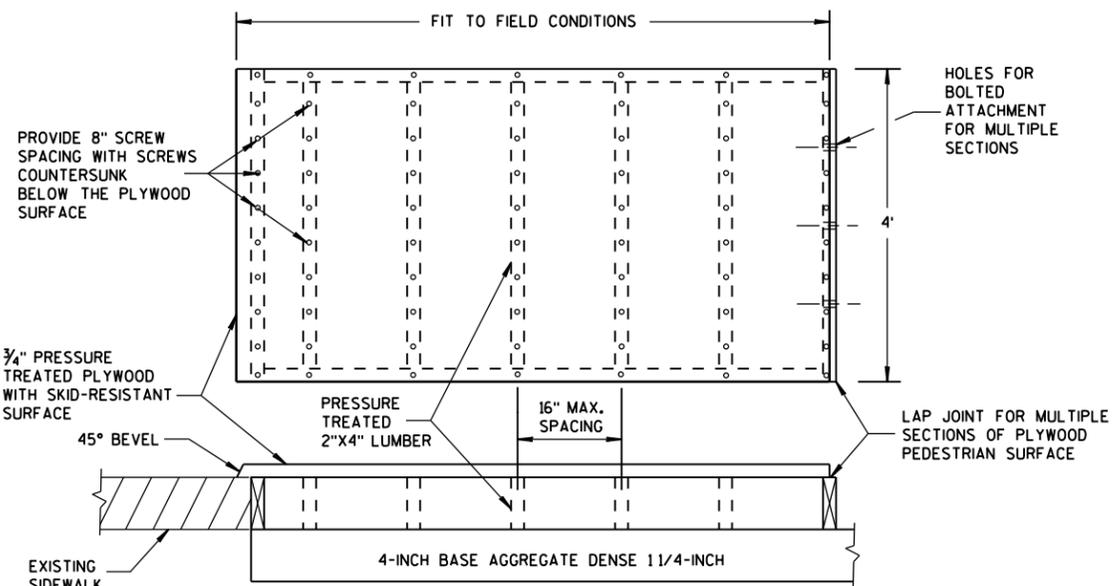
PEDESTRIAN SAFETY FENCE



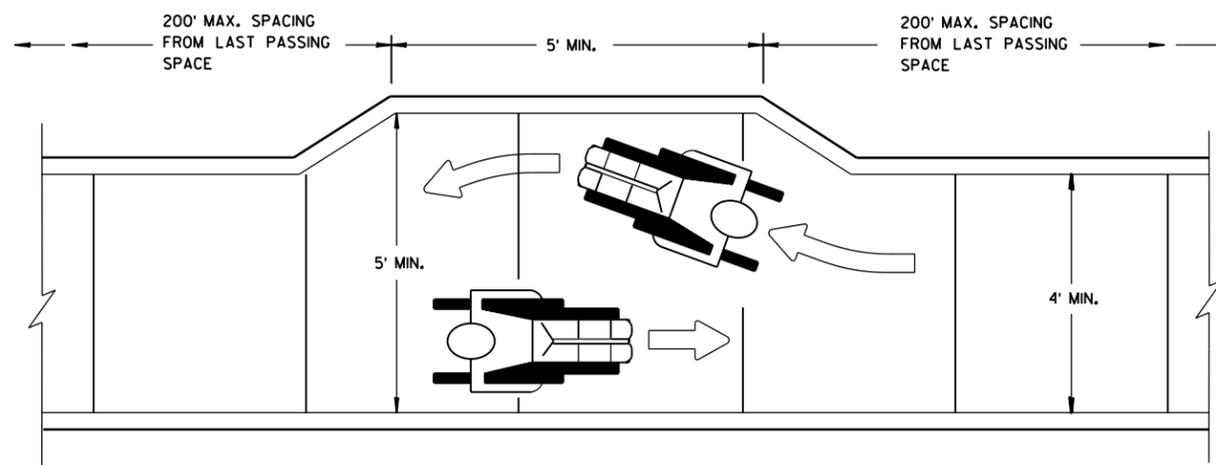
TEMPORARY PEDESTRIAN STEEL BARRICADE



TEMPORARY TYPE 3 RAMP
(OUTSIDE OF CROSSWALK AREA)



TEMPORARY PEDESTRIAN SURFACE PLYWOOD



NARROW SIDEWALK PASSING DETAIL

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED June 2016 DATE	/S/ Peter Amakobe Atepe STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER
FHWA	

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S.D.D. 15 D 30-3C

S.D.D. 15 D 30-3C

*Traffic Control, Pedestrian Accommodation***References:**

Part VI of the Manual on Uniform Traffic Control Devices (MUTCD)
[FDM 11-50-20](#)

Bid items associated with this drawing:

<u>ITEM NUMBER</u>	<u>DESCRIPTION</u>	<u>UNIT</u>
603.8000	Concrete Barrier Temporary Precast Delivered.....	LF
603.8125	Concrete Barrier Temporary Precast Installed	LF
614.0905	Crash Cushions Temporary	EACH
643.0100	Traffic Control (project)	EACH
643.0300	Traffic Control Drums	DAYS
643.0705	Traffic Control Warning Lights Type A	DAY
643.0715	Traffic Control Warning Lights Type C	DAYS
643.0900	Traffic Control Signs	DAY

Standardized Special Provisions associated with this drawing:

<u>STSP NUMBER</u>	<u>TITLE</u>
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Note: Publication of the following STSPs will not occur until June 2015.

644.1410.S	Temporary Pedestrian Surface Asphalt
644.1420.S	Temporary Pedestrian Surface Plywood
644.1430.S	Temporary Pedestrian Surface Plate
644.1601.S	Temporary Curb Ramp
644.1616.S	Temporary Pedestrian Safety Fence

Other SDDs associated with this drawing:

SDD 14b7	Concrete Barrier Temporary Precast
SDD 15c2	Barricades and Signs for Mainline Closures
SDD 15c5	Traffic Control, Advance Warning Signs 40 M.P.H. or Less, Two Way Undivided Road Open to Traffic
SDD 15d6	Traffic Control, Two Lane Two Way Operation
SDD 15d20	Traffic Control, Single Lane Closure, Non-Freeway/Expressway
SDD 15d22	Traffic Control, Two Lane Closure, Non-Freeway/Expressway
SDD 15d28	Traffic Control, Work on Shoulder or Parking Lane, Undivided Roadway

Design Notes:

Provide a staging plan showing how pedestrians will be accommodated throughout construction. When possible, divert pedestrians around construction work activities before detouring pedestrians.

When construction work activities involve sidewalks on both sides of the street, stage the construction so that both sidewalks are not closed at the same time. In the event that sidewalks on both sides of the street are closed, guide pedestrians around the work zone using a detour that has similar characteristics to that of the existing facility.

The temporary curb ramp parallel to curb should be adjusted to provide for drainage and should minimize encroachment into the vehicular travel way. In areas where pedestrian traffic is routed onto the road, provide proper barrier separation between pedestrians and traffic, especially when traffic volume and speed are high.

When mid-block sidewalk closure is required and there is sidewalk on the other side of the road, provide advance warning to pedestrians at intersection crossings. This prevents unwanted mid-block crossings. Use temporary curb ramp perpendicular to curb at temporary mid-block street crossings to direct pedestrians across the street.

Use temporary pedestrian surface in areas that may not have ADA compliant surface. Insert the temporary pedestrian surface STSP in the specials when diverting pedestrians off the sidewalk into the area on others side of the road. Limit work at an intersection to one quadrant at a time to minimize pedestrian disruptions. Provide pedestrian safety fence when necessary to separate pedestrian from construction hazards and use Type "A" flashing warning lights to delineate drop-off and other obstacles protruding in the pedestrian pathway.

Use temporary bus stop pads when the permanent bus stops are temporarily relocated. The connection from the sidewalk to the temporary bus stop pad must also use temporary pedestrian surface in areas that may

not have a proper surface such as in the terrace or construction area. Use pedestrian safety fence to separate the pedestrians from the work area.

Deactivate or cover pedestrian traffic signals heads controlling closed sidewalks and/or associated pedestrian signs.

Contact Person:

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