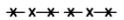
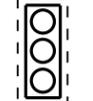


15D33: Traffic Control, One Lane Road with Temporary Signals

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  REMOVING PAVEMENT MARKING
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  CONCRETE BARRIER TEMPORARY PRECAST
-  FLAGS, 16" x 16" MIN., (ORANGE)
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
-  ASPHALTIC PAVEMENT WIDENING
-  DIRECTION OF TRAFFIC
-  4" X 6" WOOD POST
-  TEMPORARY SIGNAL WITH BACKPLATE AND 12-INCH LENSES ON BREAKAWAY POLE



W057-52
36"x24"

INSTALL ON EACH APPROACH AT THE CLOSEST INTERSECTION WITH A STATE OR COUNTY TRUNK HIGHWAY, OR AS DIRECTED BY THE ENGINEER, WIDTH ON SIGN TO BE APPROX. 1-FOOT LESS THAN AVAILABLE WIDTH. (OMIT IF AVAILABLE WIDTH IS MORE THAN 16 FEET.)

GENERAL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE FHWA'S MANUAL OF STANDARD HIGHWAY SIGNS OR THE WISCONSIN STANDARD SIGN PLATES.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

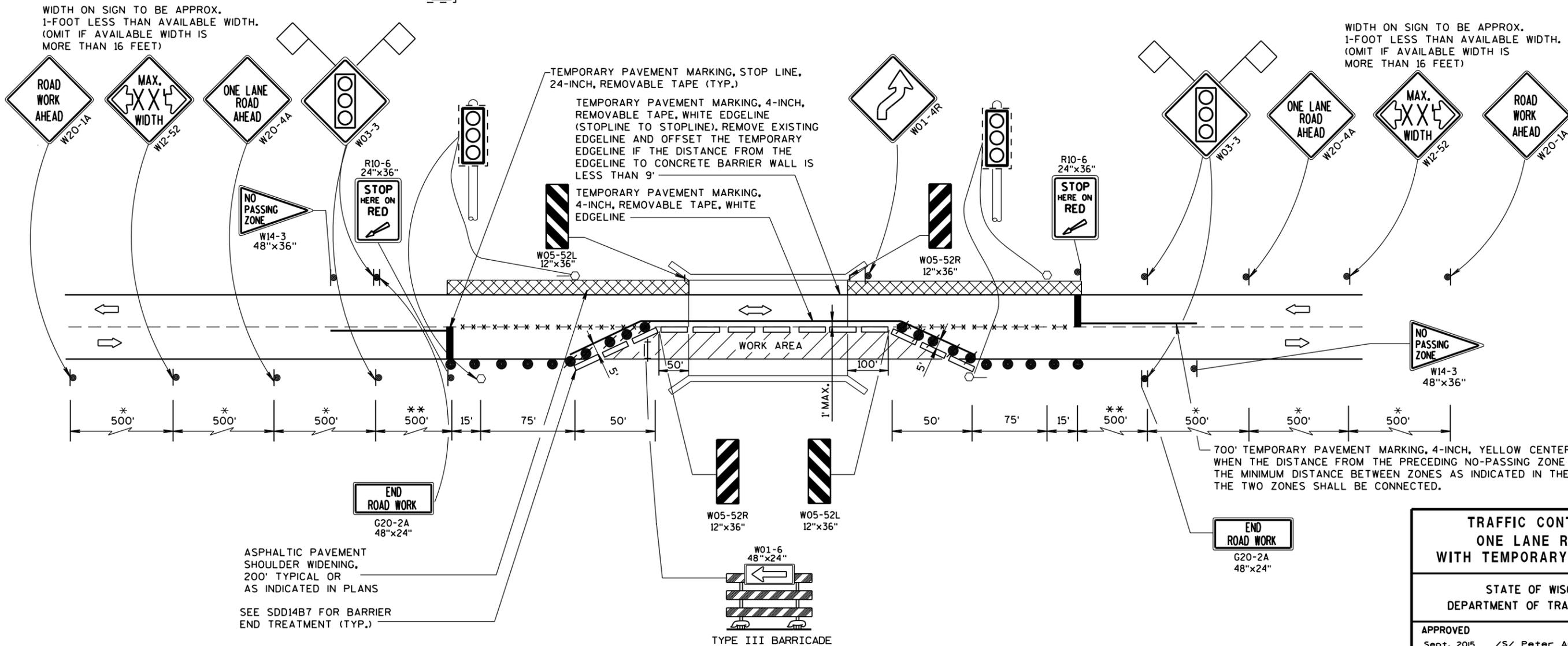
PLACE TEMPORARY PAVEMENT MARKING EDGELINE AND CENTERLINE, AND REMOVE EXISTING PAVEMENT MARKINGS IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS OR AS NOTED ON DETAIL.

* 500-FOOT SPACING SHOWN IS FOR ROADWAYS WITH A PRE-CONSTRUCTION REGULATORY SPEED LIMIT OF 45 MPH OR MORE. FOR 35-40 MPH, USE 350-FOOT TYPICAL SPACING. FOR 25-30 MPH, USE 200-FOOT TYPICAL SPACING.

** USE 300' SPACING IF PRE-CONSTRUCTION REGULATORY SPEED LIMIT IS 35 MPH OR LESS.

WIDTH ON SIGN TO BE APPROX. 1-FOOT LESS THAN AVAILABLE WIDTH. (OMIT IF AVAILABLE WIDTH IS MORE THAN 16 FEET)

WIDTH ON SIGN TO BE APPROX. 1-FOOT LESS THAN AVAILABLE WIDTH. (OMIT IF AVAILABLE WIDTH IS MORE THAN 16 FEET)



6

6

S.D.D. 15 D 33-4

S.D.D. 15 D 33-4

TRAFFIC CONTROL, ONE LANE ROAD WITH TEMPORARY SIGNALS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED DATE	/S/ Peter Amakobe Atepe STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER
FHWA	

*Traffic Control, One Lane Road with Temporary Signals***References:**

Part 6, Manual on Uniform Traffic Control Devices (MUTCD),

[FDM 11-50-1](#)[FDM 11-50-20](#)**Bid items associated with this drawing:**

<u>ITEM NUMBER</u>	<u>DESCRIPTION</u>	<u>UNIT</u>
305.0110 - 0135	Base Aggregate Dense (size).....	TON or CY
465.0125	Asphaltic Surface Temporary.....	TON
603.8000	Concrete Barrier Temporary Precast Delivered.....	LF
603.8125	Concrete Barrier Temporary Precast Installed	LF
614.0905	Crash Cushions Temporary	EACH
643.0100	Traffic Control (project)	EACH
643.0300	Traffic Control Drums.....	DAY
643.0420	Traffic Control Barricades Type III	DAY
643.0715	Traffic Control Warning Lights Type C.....	DAY
643.0900	Traffic Control Signs	DAY
646.0600	Removing Pavement Markings	LF
649.0200 - 0400	Temporary Pavement Marking Reflective/Removable 4-Inch (material)	LF
649.1400	Temporary Pavement Marking Stop Line Removable Tape 24-Inch.....	LF
661.0100	Temporary Traffic Signal for Bridges (structure)(if part of contract)	LS

Standardized Special Provisions associated with this drawing:

<u>STSP NUMBER</u>	<u>TITLE</u>
108-057	Wisconsin Lane Closure System Advance Notification

Other SDDs associated with this drawing:

SDD 9g2	Bridge Temporary Traffic Signal Installation, sheets "a" and "b" are required.
SDD 14b7	Concrete Barrier Temporary Precast

Design Notes:

Bridge Temporary Signal Installation details (SDD's 9G2-a and 9G2-b) must also be included in the contract. SDD's 15d33, 9g2-a and 9g2-b are typically used only where there are no intersecting sideroads between or near the temporary stop lines that would affect signal operation. If there are sideroads, a modified traffic control detail and modified signal installation details showing revised dimensions and/or additional signal faces for the sideroad should be used.

Traffic signal timings are dependent on the distance between the stop lines. This detail shows 50-foot and 100-foot long spaces adjacent to the bridge behind the concrete barrier for storage of contractor equipment, and 75-foot distance from end of concrete barrier to the near-right signal. These distances have typically been adequate for bridge projects. On rural roadways with high truck volume, consider the need to lengthen the distance between the stop line and end of concrete barrier to 100 feet or more. Consult with Regional Traffic Unit regarding modified distances and signal timings if signal and stop line placement or space for contractor equipment storage will vary from what is shown on this detail. If the distance between stop lines would exceed 800 feet, the duration of signal clearance time would be lengthy and could lead to significant traffic delays. If this is the case, other traffic control methods or additional stages with a limited work zone length in each stage may be needed.

If the concrete barrier cannot be flared across the traffic lane as shown, the contract should include Temporary Crash Cushions.

Where the existing paved shoulder is not wide enough for the needed shift in the path of vehicle traffic, including trucks, specify appropriate shoulder widening quantities (asphaltic surface and base aggregate), sta.-sta., and include under a "Remarks" column the widths and thickness in the Misc. Quantities table. If the widened paved shoulder will be removed upon completion of the work, use the Asphaltic Surface Temporary item. If the widened paved shoulder will become part of the permanent work, use the same Type asphalt item as is used for the permanent pavement.

If the distance between the temporary no-passing zone and the preceding no-passing zone is less than the minimum shown in Subsection 648.3.2 of the Standard Specifications, include enough quantity of temporary

pavement marking to connect the two zones. If the temporary marking is used on permanent pavement, quantities must also be included to remove the temporary marking and re-mark the needed permanent marking when the project is completed. As an option for smaller quantities of no-passing marking, removable tape may be used.

Contact Person:

Peter Amakobe Atepe (608) 261-0138