

2024 TS4 Permit Report

To Comply with the Reporting Requirements of the
WDNR Transportation Separate Storm Sewer System

(TS4) Permit

For

Stormwater Management

WPDES General Permit No. WI-S066800-2

Prepared September 2025



TABLE OF CONTENTS

1 Introduction..... 6

1.1 Cooperative Agreement 12

1.1.1 Updates to Cooperative Agreement 12

1.1.2 WDNR Transportation Liaison Funding..... 12

1.1.3 Stormwater and Erosion Control Engineers (SWECE) 12

1.1.4 Roles and Responsibilities 12

2 Stormwater Management Programs13

2.1 Public Education and Outreach Program 13

2.1.1 Stormwater Quality Training..... 13

2.1.2 Summary of DOT Stormwater Management Program at PIMs..... 14

2.1.3 DOT's Written Publication of Stormwater Management Program 15

2.1.4 Research 15

2.1.5 Education..... 15

2.1.6 Measurable Goals..... 15

2.2 Public Involvement and Participation Program 15

2.2.1 Stormwater Guide Brochures 16

2.2.2 Stormwater Knowledgeable Staff 16

2.2.3 Local Outreach 16

2.2.4 Adopt-A-Highway Program 16

2.2.5 Public Comment on Annual Report 18

2.2.6 Municipal Meetings 18

2.2.7 Standards Oversight Council (SOC) 18

2.2.8 National Cooperative Highway Research Program 19

2.2.9 Measurable Goals..... 19

2.3 Illicit Discharge Detection and Elimination Program 19

2.3.1 Prevention and Elimination of Illicit Discharges and Connections..... 19

2.3.2 Illicit Discharge Documentation 20

2.3.3 Summary of Completed Screening 21

2.3.4 Illicit Discharge and Spill Response Training..... 21

2.3.5 Record of Illicit Discharges..... 21

2.3.6 Unauthorized Discharges..... 22

2.3.7 Goals for Illicit Discharge Detection and Elimination Program 22

2.4 Construction Site Pollutant Control Program 22

2024 TS4 Permit Report

2.4.1	Submit Plans to DNR TL.....	23
2.4.2	Facilities Development Manual Updating	23
2.4.3	Erosion and Sediment Control Compliance Procedure	24
2.4.4	Measurable Goals.....	26
2.5	Post-Construction Stormwater Management Program	26
2.5.1	TCGP Conditions.....	26
2.5.2	FDM Ch. 10 Erosion Control and Stormwater Quality Updates	26
2.5.3	Drainage Area Modeling Procedure	26
2.5.4	SCP Inventory.....	27
2.5.5	Goals for Post Construction Stormwater Management Program	29
2.6	Pollution Prevention Program	29
2.6.1	DOT Highway Runoff Management	29
2.6.2	Stormwater Pollution Prevention Plans (SWPPPs)	30
2.6.3	Training.....	30
2.6.4	Updates	30
2.6.5	Measurable Goals.....	30
2.7	Stormwater Quality Management.....	30
2.7.1	20% TSS Reduction Goal Status	30
2.7.2	Retrofit Stormwater Control Practices	31
2.8	Storm Sewer System Maps.....	32
2.9	Annual Report.....	33
2.9.1	Status of 6 Stormwater Program Areas.....	33
2.9.2	Other Entities	34
2.9.3	Updated Mapping	34
2.9.4	Water Quality Degradation.....	34
2.9.5	Summary of TMDL Actions	34
2.9.6	Signer	34
2.10	Reapplication for Permit Coverage	34
2.11	Amendments.....	35
3.0	TMDL Requirements.....	35
3.1	Tabular Summary of TMDL Reachshed.....	35
3.2	List of Projects Expected to Require TCGP Coverage Within the TS4	35
3.3	Anticipated Progress Towards TMDL Wasteload Allocation	35
3.4	Updated Summary of Pollutant Reduction for Each TMDL.....	35

3.5	TMDL Compliance and Implementation for Bacteria WLAs.....	35
3.5.1	Milwaukee River TMDL Bacteria Inventory and Map.....	35
3.5.2	Collaboration with Adjacent Municipalities.	35
4.0	Implementation Schedule	36
5.0	Signatory Page	40

Figures:

Figure 1.1:	North Central Region Stormwater Overview	7
Figure 1.2:	Northeast Region Stormwater Overview	8
Figure 1.3:	Northwest Region Stormwater Overview	9
Figure 1.4:	Southeast Region Stormwater Overview	10
Figure 1.5:	Southwest Region Stormwater Overview	11
Figure 2.3.2:	Illicit Discharge Field Screening Form	20

Tables:

Table 2.2:	2024 PIM Summary	16
Table 2.2.4.1:	Adopt-A-Highway Program - Number of Volunteers by Year	17
Table 2.2.4.2:	Adopt-A-Highway Program - Number of Trash Bags Collected by Year	18
Table 2.2.4.3:	Adopt-A-Highway Program - Number of Recycling Bags Collected by Year	18
Table 2.5.4.1.3.1:	SCP Inspections Completed by Year and Region	28
Table 2.5.4.1.3.2:	Urgent Repairs Required by Year and Region	28
Table 2.7.1:	Statewide Runoff Volume and Pollutant Reduction Summary	31
Table 2.7.2:	Stormwater Projects Coordinated with Other Agencies	32
Table 2.9.1.3:	Stormwater Research Initiatives	33
Table 2.9.1.4:	Fiscal Analysis	34

2024 TS4 Permit Report Appendix Sections

Section A – Highway Runoff Management Activities

Section B – WisDOT Pollutant Loading Analysis Process

Section C – Research Project Summaries

Section D – Statewide High-Risk Outfall and Stormwater Control Practice Locations

Section E – Statewide Storm Sewer System Mapping Study Area Subbasins

Section F – SCP Inspection Forms and Maintenance Guidelines

Section G – Pollutant Loading Analysis Detail by County

Section H – WisDOT Statewide Stormwater Control Practice Database

Section I – Typical Highway and Freeway Cross Sections

Section J – List of Non-Highway WisDOT SWPPP Facilities

Section K – Milwaukee River Bacteria TMDL Pilot Study Report

Section L – Additional DOT Developed Measurable Goals

TS4 Permit Conditions

1 Introduction

The Wisconsin Department of Transportation (WisDOT) has prepared this report to describe how WisDOT has met the Transportation Separate Storm Sewer System (TS4) permit (WPDES-WI-S066800-2) obligations in calendar year 2024.

This report is structured by permit section to respond to each of the TS4 Permit requirements. Source data references in this report are defined as Sections followed by a letter label and found in the Appendix.

Figures 1.1-1.5 on the following pages provide an overview of each WisDOT Region. They locate the state, U.S., and interstate highways, urbanized areas, Stormwater Pollution Plan Prevention (SWPPP) sites, high risk outfalls, and the stormwater control practices (SCPs) that WisDOT is responsible to inspect and maintain.

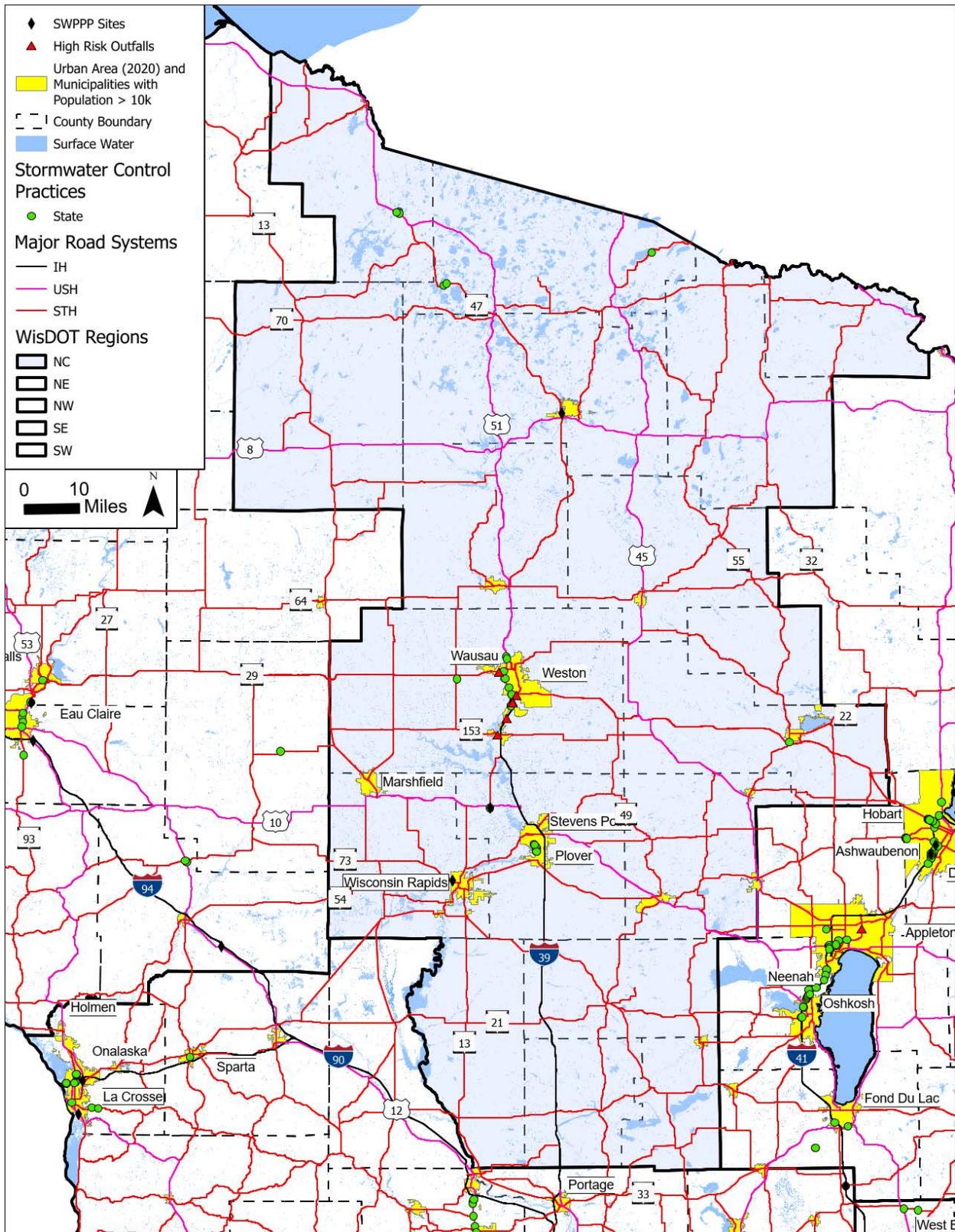


Figure 1.1 – North Central Region Stormwater Overview

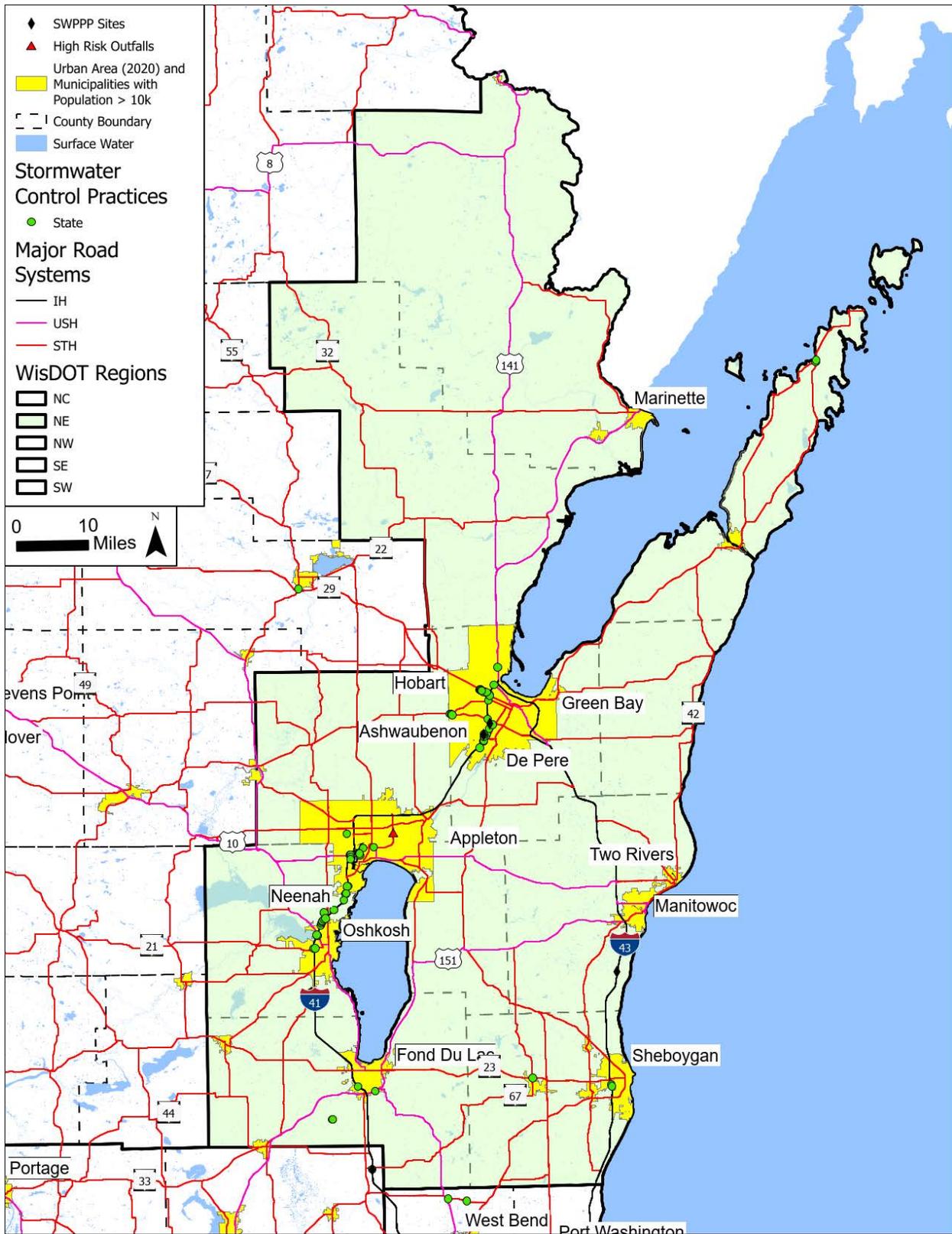


Figure 1.2 – Northeast Region Stormwater Overview

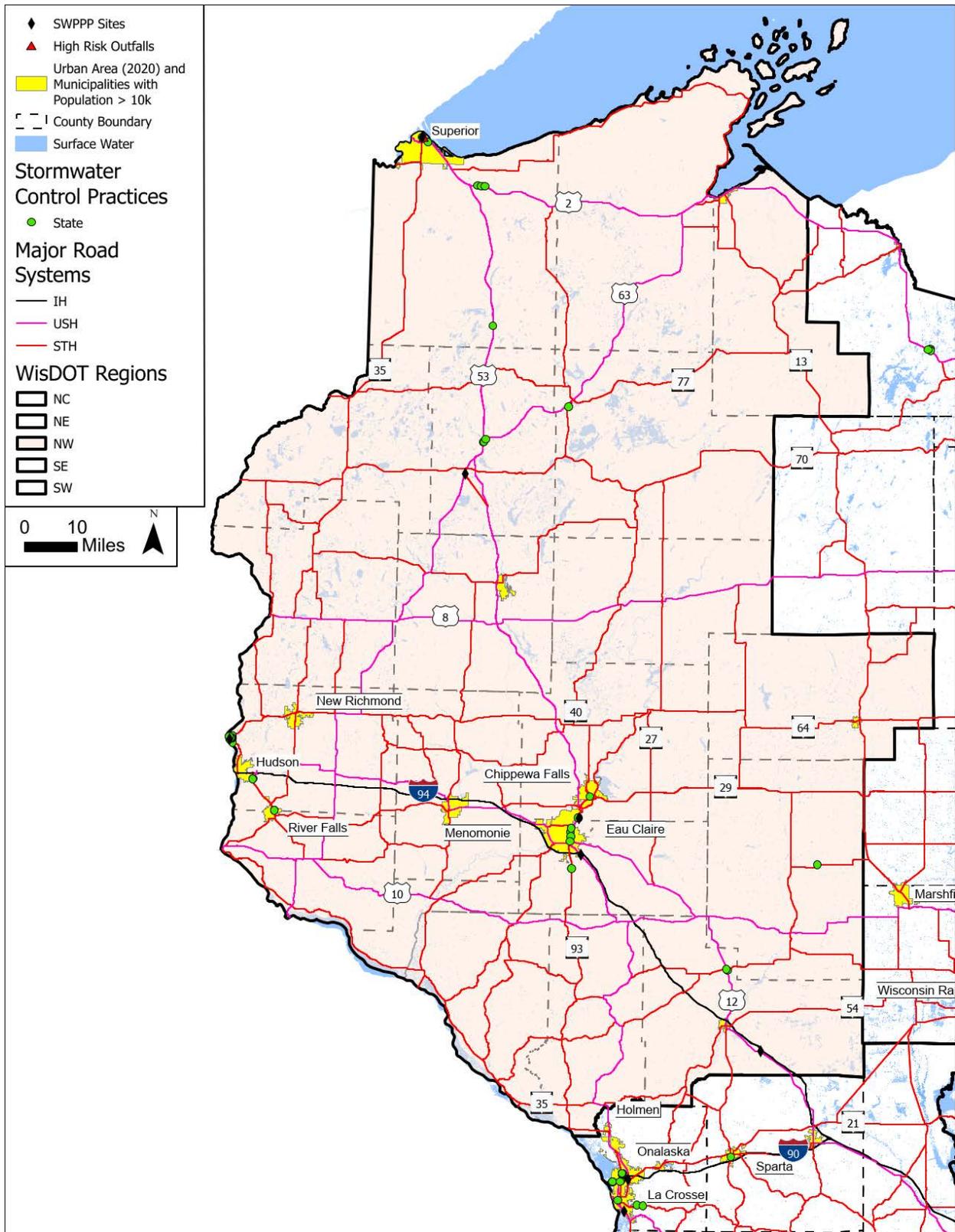


Figure 1.3 – Northwest Region Stormwater Overview

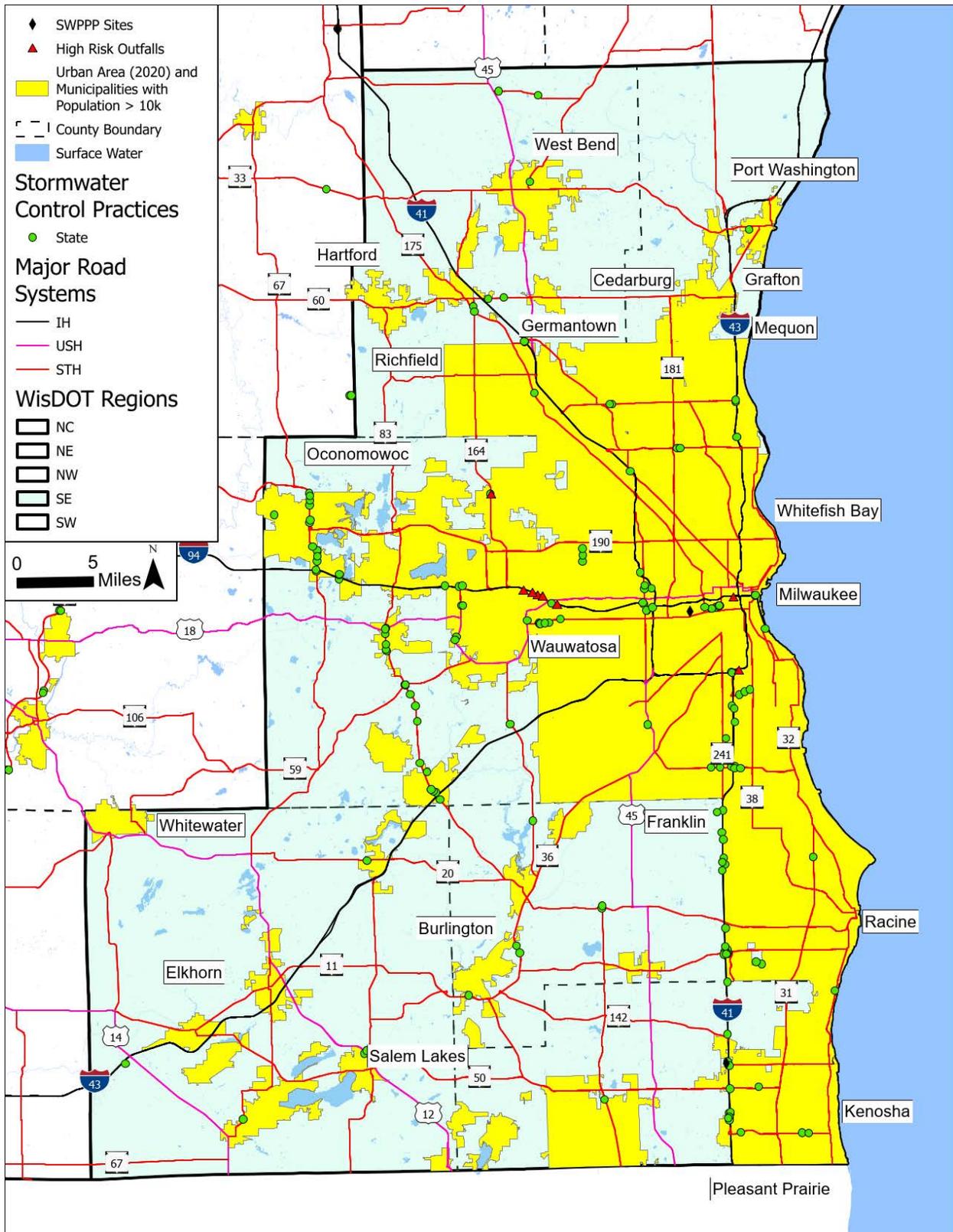


Figure 1.4 – Southeast Region Stormwater Overview

1.1 Cooperative Agreement

The Department and the Wisconsin Department of Natural Resources (WDNR) continue to utilize the Cooperative Agreement (CoA) as the overarching interdepartmental liaison procedures for transportation activities that are carried out under the direction and supervision of DOT, as established in Section 30.2022, Wis Stats.

1.1.1 Updates to Cooperative Agreement

Both agencies continued to work on updating the Floodplain & Waterway CoA Attachment in 2024. The goal is to finalize this Attachment by the end of 2025.

In late 2024, the agencies initiated an effort to review and update the main DNR-DOT Cooperative Agreement to meet expectations in Section 5.2 of the agreement. This effort will be ongoing through 2025 with the goal to finalize by the end of calendar year 2025.

The CoA and CoA Attachments are located on this Department webpage under the Wisconsin Department of Natural Resources heading: [Wisconsin Department of Transportation Forms and Tools \(wisconsindot.gov\)](https://wisconsin.gov/transportation/forms-and-tools)

1.1.2 WDNR Transportation Liaison Funding

WisDOT is continuing to fund WDNR Transportation Liaison (WDNR TL) staff statewide.

The WDNR TLs review and work on all the environmental areas, not just TS4 related tasks, so there is not a way to break down the funding for just TS4 work.

In total, we provided \$2.23 million in FY24 and \$2.344 million in FY25. This includes the transportation liaison funding (\$1.55 million), Local Roads Improvement Program (LRIP) funding (\$500k), hazmat funding (\$235k) and endangered species funding (\$58k).

1.1.3 Stormwater and Erosion Control Engineers (SWECE)

WisDOT currently funds 10.5 permanent full-time Regional and Central Office Stormwater & Erosion Control staff.

1.1.4 Roles and Responsibilities

The roles and responsibilities of the WDNR TLs and the WisDOT SWECEs are listed below:

1. *Project Planning.* Projects are planned, designed, constructed, and maintained according to the requirements of the permits.
2. *WDNR Notification.* WisDOT notifies WDNR Liaisons of proposed construction as early as possible in the planning process.
3. *WisDOT/WDNR Semi-Annual Meetings.* Staff members from both agencies meet at least twice per year to discuss current projects and issues.
4. *WDNR Reviews.* The WDNR Liaisons review proposed projects to make sure they comply with applicable regulatory and environmental requirements.
5. *WDNR Recommendation Consideration.* WisDOT considers WDNR Liaison project recommendations and resolves any differences according to the processes described in the Cooperative Agreement.
6. *Design Study Reports.* Agreed upon recommendations are included in project Design Study Reports.
7. *Environmental Commitments.* SWECEs and WisDOT project engineers monitor projects to ensure that the environmental commitments and regulatory requirements are met.

2 Stormwater Management Programs

2.1 Public Education and Outreach Program

2.1.1 Stormwater Quality Training

WisDOT Bureau and Regional staff that focus their efforts on construction erosion and sediment control and post-construction stormwater management provided and participated in the following stormwater-related trainings in 2024:

WisDOT Provided Trainings

The following training was provided by WisDOT Bureau of Technical Services – Environmental Services Section (BTS – ESS) Central Office Engineers and regional Stormwater & Erosion Control Engineers (SWECEs) during 2024.

WisDOT Provided Statewide Erosion Control Training

BTS – ESS and regional Stormwater & Erosion Control Engineer's (SWECE's) team provided statewide Erosion Control training to approximately 400 engineers and technical specialists in all 8 offices in January – March 2024. Attendees included WisDOT staff, consultant staff responsible for construction oversight of WisDOT projects and DNR Transportation Liaisons (DNR TLs). This was the first in-person Statewide Erosion Control Training provided by WisDOT since 2016. It was a massive effort led by the WisDOT Statewide Erosion Control & Sediment Engineer. For their efforts the team recently received the WisDOT Three Pillars of Excellence award. Here is a small excerpt from the award -

“They set the model for environmental training efforts with their collaborative approach to developing content and delivering the training, as well as the engaging content and event logistics. It was a substantial effort that set the construction program up for continued success.”

The department intends to continue providing erosion control training on an annual basis with an emphasis on expanding the knowledge base of our construction field staff.

WisDOT Provided Regional Training and Technical Expertise

The regional SWECEs typically provide an abbreviated erosion control training at each region's spring construction conference to ensure construction staff know who to contact and to emphasize critical elements. WisDOT provides one-on-one mentoring to field and office staff, deliver, as necessary, stormwater related trainings and provide critical information to staff, project managers and management.

Other WisDOT Provided Trainings

WisDOT presents a high-level environmental review, including construction related points of emphasis, at the annual Wisconsin Transportation Builders Association (WTBA).

WisDOT provides TS4 and Transportation Construction General Permit (TCGP) related trainings at the monthly environmental staff meetings, as needed.

Trainings Participated in by WisDOT Staff

BTS – ESS does not track trainings attended by regional Project Development Section (PDS) staff. However, WisDOT does track trainings by BTS – ESS engineers and regional SWECEs.

WisDOT BTS - ESS – Central Office

WisDOT Statewide Stormwater Quality Engineer:

- International Erosion Control Association (IECA) Annual Conference – Spokane, WA
- North American Stormwater and Erosion Control Assoc. (NASECA) Rice Lake 2024 Field Event
- Advanced Drainage System (ADS) Culvert & Storm Drain Inspection Guide
- PE Ethics Workshop: Ethical Considerations in Water, Wastewater & Stormwater – UW-Madison, WI
- Wisconsin Association for Floodplain, Stormwater & Coastal Management (WAFSCM) 2024 Annual Conference

WisDOT Statewide Erosion Control & Sediment Engineer:

- IECA Annual Conference – Spokane, WA
- IECA Webinar - Understanding RUSLE Analysis
- NASECA 21st Annual Conference & Trade Show
- NASECA Rice Lake 2024 Field Event

WisDOT Regional SWECEs

NER SWECE

- Stormwater Conference – Fox/Wolf Watershed
- NASECA Rice Lake 2024 Field Event

NWR SWECE

- NASECA 2024 Construction Site Erosion Control and Stormwater Permit Compliance Training

SWR SWECE

- NASECA 2024 Construction Site Erosion Control and Stormwater Permit Compliance Training

SWR SWECE – La Crosse

- NASECA Rice Lake 2024 Field Event

2.1.1.1 Design, Implementation & Maintenance of Construction BMPs

As discussed above, DOT hosted in-person Construction Erosion Control Trainings at 8 locations (Rhineland, Wisconsin Rapids, Green Bay, Eau Claire, Superior, La Crosse, Waukesha, and Madison) in 2024.

2.1.2 Summary of DOT Stormwater Management Program at PIMs

The DOT is prepared to present a summary of its Stormwater Management Program to the public in the form of an educational guide at PIMs and associated public officials meeting for

100% of its mega projects, 50% of its major projects, and at PIMs with projects that have a substantial stormwater component.

2.1.3 DOT's Written Publication of Stormwater Management Program

WisDOT developed a stormwater management program educational guide and has these stockpiled in the regions and at the BTS-ESS office to be available as handouts at PIMs as described in 2.1.2. There is also a link to the guide on the WisDOT Stormwater Management webpage - [Wisconsin Department of Transportation Stormwater Management \(wisconsindot.gov\)](https://wisconsin.gov/transportation/stormwater-management)

The guide briefly summarizes the main elements of WisDOT's stormwater program and includes how the public can help protect public waters.

2.1.4 Research

The DOT's Statewide Stormwater Quality Engineer has requested to be involved in an National Cooperative Highway Research Program (NCHRP) Panel.

2.1.5 Education

DOT supports staff involvement in stormwater trade associations.

The Statewide Erosion and Sediment Control Engineer serves on the board of directors for the North American Stormwater and Erosion Control Association of Wisconsin (NASECA), which is a collaboration of professionals from around the state, including WisDOT, DNR, municipal, and manufacturer personnel.

As discussed in Section 2.1.1, staff involved in stormwater-related activities participated in various educational events held by NASECA.

The Statewide Stormwater Quality Engineer participates in the American Association of State Highway and Transportation Officials (AASHTO) Committee on Environmental and Sustainability Stormwater Working Group meetings. The meeting focuses on stormwater issues that state DOTs work on.

2.1.6 Measurable Goals

To meet the conditions of this section, the "Additional DOT Developed Measurable Goals" with tentative schedule is included in **Section L**.

2.2 Public Involvement and Participation Program

WisDOT's Facility Development Manual (FDM) Section 6-5-25.1 describes the Public Involvement Meetings (PIMs) requirements.

Table 2.2 – 2024 PIM Summary

Region	NWR
Design or Construction ID	1199-00-08
General Project Limits	I-535/Garfield Ave Interchange to I-535/US 53/Hammond Ave/WIS 35 Interchange
Type of Project	Mega – part of MnDOT led Blatnik Bridge project
PIM Date	10/15/2024 & 05/15/2024
Brochures Available	Available online
Brochures Handed Out	0
Design PM	Beth Cunningham
Stormwater Staff Available	No
Remarks	No. The stormwater design was not worked on until 2025. There are PIMs planned for 2025 and now that the project has an idea of the stormwater implications, brochures will be available for the public.

2.2.1 Stormwater Guide Brochures

WisDOT developed a stormwater management program educational guide brochure and has these available in the regions and at the BTS-ESS office to be available as handouts at PIMs. There is also a link to the guide brochure on the WisDOT Stormwater Management webpage - [Wisconsin Department of Transportation Stormwater Management \(wisconsin.gov\)](https://wisconsin.gov/transportation/stormwater-management)

The brochure was available at the WisDOT booth at the Wisconsin State Fair in August 2024 with approximately 150 copies handed out.

2.2.2 Stormwater Knowledgeable Staff

FDM Section 6-5-25.1 describes how project staff familiar with the project’s stormwater design should attend the public involvement meeting to be available for public questions.

2.2.3 Local Outreach

The department has limited staff available to participate in local outreach at this time. The department will look and plan for opportunities to participate in local outreach as part of developing the measurable goals in 2.2.9.

2.2.4 Adopt-A-Highway Program

WisDOT’s Adopt-a-Highway Program has regional coordinators organized by County and with the Program Manager in the Bureau of Highway Maintenance (BHM).

WisDOT's Adopt-A-Highway program is the official coordination system for generous volunteer efforts that remove trash, recyclables, and litter from approximately 9,500 miles of the roughly

2024 TS4 Permit Report

11,800 total miles of state highways in Wisconsin. This program has been in place since 1990 with the aim of creating public awareness of roadside litter, initiating public involvement in environmental stewardship, and regularly cleaning up litter along highways. The Adopt-A-Highway program operates throughout the state, not only in areas within TS4 limits.

Although this program does not manage volunteer cleanup work on local and county roads, interested parties are welcome to help organize community efforts. Each qualified group takes responsibility for litter control on approximately a two-mile segment of state highway. The groups pick up litter on this segment at least two times per year between April 1 and November 1. The trash bags full of collected litter are left on roadsides once the clean-up is completed, which are then collected by county staff who are notified by the Adopt-A-Highway group. The counties charge costs for sign installation and collection and disposal of trash bags to WisDOT for reimbursement. Any materials deemed hazardous (medical waste, weapons, etc.) are not moved by volunteers; instead, local authorities are contacted at their non-emergency number. After a cleanup event, the group submits a report through the Adopt-A-Highway webpage listing the number of trash bags collected, number of participants, and hours spent on the section. This data is used by WisDOT to summarize the number of trash and recycling bags collected, number of participants, and other details for WisDOT to track program efforts and set practical goals for the future.

It is important to be very careful when working on the roadsides, so the Adopt-A-Highway program emphasizes safety. WisDOT furnishes safety vests, portable "people working" signs, the safety training video, trash bags, trash bag pick-up and disposal, and two acknowledgment signs marking the adopted highway section. Groups do not work in dangerous areas like medians, bridges, or steep slopes. WisDOT installs the signs announcing each group's litter control sponsorship. No fees will be charged to groups accepted into the Adopt-a-Highway program, and work is permitted until the group notifies the Department of their intent to abandon their segment. More information is available at [Wisconsin Department of Transportation Adopt-A-Highway program \(wisconsindot.gov\)](https://www.wisconsin.gov/transportation/adopt-a-highway) and in the WisDOT Maintenance Manual - [Highway Maintenance Manual \(HMM\) 07-01-25 Adopt-A-Highway \(wisconsindot.gov\)](#).

The Bureau of Technical Services has recently learned from the BHM the extent of volunteers and quantity of trash and recyclable materials the Adopt-a-Highway program has been removing from roadways. BHM has been tracking these activities for the last few years with the information summarized in the following tables:

Table 2.2.4.1: Adopt-A-Highway Program - Number of Reported Volunteers by Year

Region	2020	2021	2022	2023	2024
NCR	0	25	2,459	2,481	2,687
NER	0	85	3,143	3,209	3,050
NWR	27	62	1,773	3,040	2,804
SER	0	75	2,101	2,606	2,521
SWR	247	484	2,445	4,041	5,047
Total	274	731	11,921	15,377	16,109

Note: Number of volunteers was just starting to be collected in 2020.

Table 2.2.4.2: Adopt-A-Highway Program - Number of Trash Bags Collected by Year

Region	2020	2021	2022	2023	2024
NCR	2,469	3,646	3,603	3,757	3,488
NER	2,375	3,333	3,096	3,227	2,824
NWR	1,190	1,217	1,842	3,330	3,346
SER	1,187	2,045	2,078	2,601	2,226
SWR	1,598	2,263	2,976	4,365	5,062
Total	8,819	12,504	13,595	17,280	16,946

Table 2.2.4.3: Adopt-A-Highway Program - Number of Recycling Bags Collected by Year

Region	2020	2021	2022	2023	2024
NCR	134	177	272	247	248
NER	129	196	180	130	163
NWR	153	108	196	288	419
SER	46	108	117	213	129
SWR	113	167	194	280	385
Total	575	756	959	1,158	1,344

Note that the locations for the Adopt-a-Highway program are not all within the TS4 areas and have not been tracked to that extent.

2.2.5 Public Comment on Annual Report

The TS4 Annual Reports are listed on the DOT’s stormwater webpage, found here - [Wisconsin Department of Transportation Stormwater Management \(wisconsindot.gov\)](https://www.wisconsin.gov/transportation/stormwater)

There were no public comments on the 2023 TS4 Annual Report through the website in 2024.

2.2.6 Municipal Meetings

WisDOT will provide an opportunity and/or a forum to conduct regional/municipal stormwater planning meetings during the permit term to discuss potential SCPs that could provide treatment for the Municipal Separate Storm Sewer Systems (MS4’s), the TS4, local stormwater quality concerns and maintenance of shared SCPs. MS4’s that may be included are listed in the TS4 Permit Appendix A.

WisDOT regions engage the local municipalities when projects impact them and discuss stormwater issues.

2.2.7 Standards Oversight Council (SOC)

The Wisconsin Standards Oversight Council (SOC) is an inter-agency collaboration that facilitates a team approach to develop and maintain technical standards for soil and water conservation practices in Wisconsin that help protect Wisconsin’s natural resources. The SOC Technical Standards Process Handbook documents the step-by-step processes for developing and revising technical standards, as well as related SOC policies and procedures.

The Memorandum of Agreement (MOA), signed by the DOT Secretary and the leads of the other agencies, discusses the roles and responsibilities of the SOC members.

The Statewide Stormwater Quality Engineer and Statewide Products Engineers are SOC Advisory Committee members and attend the committee meetings and review and comment on the Technical Standards being developed by SOC. Reviews of Technical Standards are also performed by other DOT technical staff.

DOT technical staff have served as team members for the technical standards developed through the SOC process.

2.2.8 National Cooperative Highway Research Program

As discussed in the 2023 TS4 Annual Report, the Statewide Stormwater Quality Engineer was a panel member in the development of the NCHRP Synthesis – “Outsourcing Post-Construction Stormwater Best Management Practice Inspection and Maintenance Activities” NCHRP Project 20-05, Topic 54-02, during 2022 - 2023. The report was finalized in late 2023 and published in early 2024. A copy of the final report is referenced in the 2023 TS4 Report in Section 2.7, titled Transportation Research Board (TRB) Participation. The report is in Section M of the 2023 TS4 Report Appendices.

2.2.9 Measurable Goals

To meet the conditions of this section, the “Additional DOT Developed Measurable Goals” with tentative schedule is included in **Section L**.

2.3 Illicit Discharge Detection and Elimination Program

An illicit discharge is any discharge to a TS4 that is not composed entirely of stormwater except discharges authorized by a WPDES permit or other discharge not requiring a WPDES permit such as landscape irrigation, individual residential car washing, firefighting, diverted stream flows, uncontaminated groundwater infiltration, uncontaminated pumped groundwater, discharges from potable water sources, foundation drains, air conditioning condensation, irrigation water, lawn watering, flows from riparian habitats and wetlands, and similar discharges.

2.3.1 Prevention and Elimination of Illicit Discharges and Connections

WisDOT completed an inspection for illicit discharges from all major outfalls identified during the most recent field mapping reviews, which were conducted in 2013. New major outfalls will be inspected in 2026 after the mapping work for the current permit is complete. There are currently no known WisDOT major outfalls that have not been inspected. If illicit discharges are discovered during these inspections, they will be reported in Section 2.3.2 of this report.

The Department continues to focus on preventing illicit discharges by implementing the required construction site erosion control practices for each project, performing the outfall inspections as described earlier, reviewing private project’s stormwater management and development plans that convey drainage onto WisDOT ROW, performing normal roadway maintenance inspections and working with property owners and local governments to correct issues.

2.3.2 Illicit Discharge Documentation

WisDOT uses a one-page form to document the illicit discharge inspection performed for each major outfall when performing stormwater mapping development. The form is shown in Figure 2.3.2 below.

FIELD SCREENING—VISUAL OBSERVATION			
Inspector	<input style="width: 100%; height: 20px;" type="text"/>	Last Rainfall Date	<input style="width: 100%; height: 20px;" type="text"/>
DATE	<input style="width: 100%; height: 20px;" type="text"/>	TIME	<input style="width: 100%; height: 20px;" type="text"/>
Field Outfall #	<input style="width: 100%; height: 20px;" type="text"/>	LOCATION	<input style="width: 100%; height: 20px;" type="text"/>
Final Outfall #	<input style="width: 100%; height: 20px;" type="text"/>	WisDOT Culvert /Bridge Plate #	<input style="width: 100%; height: 20px;" type="text"/>
Outfall Type	<input style="width: 100%; height: 20px;" type="text"/>	Outfall Size	<input style="width: 100%; height: 20px;" type="text"/>
Is There A Flow Present? <input type="checkbox"/> Yes <input type="checkbox"/> No Est. Flow Rate _____			
If there is a flow, note the following and provide a description as needed			
Color:	<input type="checkbox"/> Clear	<input type="checkbox"/> Yellow	<input type="checkbox"/> Grey <input type="checkbox"/> Brown <input type="checkbox"/> Green <input type="checkbox"/> _____
Turbidity:	<input type="checkbox"/> Clear <input type="checkbox"/> Slightly Cloudy <input type="checkbox"/> Cloudy <input type="checkbox"/> Opaque		
Pollutant Type:	<input type="checkbox"/> None <input type="checkbox"/> Sediment <input type="checkbox"/> Chemical <input type="checkbox"/> Biological <input type="checkbox"/> Uncertain		
Surface Sheen:	<input type="checkbox"/> None <input type="checkbox"/> Oil <input type="checkbox"/> Gasoline <input type="checkbox"/> Scum <input type="checkbox"/> Unknown		
Deposits/Stains:	<input type="checkbox"/> None <input type="checkbox"/> Sediment <input type="checkbox"/> Oily		
Odor:	<input type="checkbox"/> None <input type="checkbox"/> Oil <input type="checkbox"/> Paint <input type="checkbox"/> SO ₂ <input checked="" type="checkbox"/> Fuel <input type="checkbox"/> Sewage <input type="checkbox"/> Decaying Vegetation <input type="checkbox"/> Unknown		
Damage to Outfall Structure:	<input type="checkbox"/> None <input type="checkbox"/> Concrete Cracking <input type="checkbox"/> Concrete Spalling <input type="checkbox"/> Peeling Paint <input type="checkbox"/> Metal Corrosion <input type="checkbox"/> Other _____		
Extent: _____			
DESCRIBE ANY OTHER RELEVANT OBSERVATIONS REGARDING POTENTIAL PRESENCE OF NON-STORM WATER DISCHARGES OR ILLEGAL DUMPING			
<input style="width: 100%; height: 20px;" type="text"/>			
<input style="width: 100%; height: 20px;" type="text"/>			
<input style="width: 100%; height: 20px;" type="text"/>			

Figure 2.3.2 – Illicit Discharge Field Screening Form

If a suspected illicit discharge is detected during the mapping process, it would be immediately reported to WisDOT Stormwater Engineer (SWECE) and the Project Coordinator of the Region for additional investigation. The inspector is directed to use the link [How to report a spill || Wisconsin DNR](#) to obtain WDNR form 4400-225, titled “Notification for Hazardous Substance Discharge (Non-Emergency Only)”. This form would be completed for each suspected illicit

discharge and submitted to the mapping coordinator, the appropriate SWECE, the WDNR Transportation Liaison for the WisDOT region of the discharge [Liaisons.pdf \(widen.net\)](#), and to the mapping Project Coordinator within 24 hours of discovery. The Project Coordinator would submit the completed WDNR form 4400-225 to the Central Office Hazmat staff at DOTHazmatUnit@dot.wi.gov to initiate the investigation of the suspected illicit discharge.

The data collected for outfall screening is as described in the permit and using the “Field Screening – Visual Observation Form” as shown in Figure 4.5. All major outfall screenings were completed with the statewide mapping activities conducted as part of the past MOU requirements and as required for the TS4 permit. Going forward, illicit discharge reporting and urgent maintenance reporting will follow the guidelines described in **Sections J and K**.

2.3.3 Summary of Completed Screening

WisDOT was provided with stormwater outfall mapping locations, in GIS format, from a variety of consultants during the 2002 – 2019 time period. From this information, the Department identified all of the “High Risk” outfalls from this original source mapping.

High risk outfalls were defined as outfalls that are 36” or more in diameter and adjacent to an industrial facility. Aerial photos were used to identify the type of industrial facility and to determine whether or not the outfall should be identified as High Risk. A quality control review of these sites was performed with senior engineering staff.

Further field inspection is now required for follow up of these sites. Per the TS4 Permit, each year, 20% of identified outfalls must undergo inspection. These inspections are completed alongside annual stormwater control practices for efficiency and will be reinspected every five years. The “High Risk” outfalls will be evaluated using the illicit discharge forms attached in the appendices.

The results identified a total of 22 “High Risk” outfalls statewide that met these criteria. The outfalls are located on the mapping included in **Section D**. Each outfall designation was assigned a unique ID, outfall number, highway designation that the outfall is located near, County name in which the outfall is located, WisDOT Region in which the outfall is located, and X and Y coordinates (in NAD83 HARN Wisconsin TM meters).

Inspectors will be provided with the Regional Stormwater Mapping pdf figures from **Section D** that identify the locations of these “High Risk” outfalls within each WisDOT Region.

2.3.4 Illicit Discharge and Spill Response Training

The Department did not provide illicit discharge training in 2024 for stormwater mapping development or stormwater control practice inspections. The goal is to provide this training in the future using the illicit discharge reporting guidance described in Section 6 - Fieldwork and Desktop Protocols, in the Stormwater Program Guidance Manual.

2.3.5 Record of Illicit Discharges

SCP inspections completed in 2024 resulted in two instances of reporting a possible illicit discharge. The following is a summary of the reported potential illicit discharges.

1. During the SCP inspection along USH/IH 41 in Fond du Lac County on June 24, 2024, the inspection team found a chalky orange substance at one of the dry detention pond

(SCP 286) inlets. The site was re-inspected on August 5, 2024, and the substance was determined to be natural organic material – cattail pollen and algae residue. No further action was taken because the material appeared to be a natural organic material and because the only observed location of the material was at an inlet to the pond.

2. During the SCP inspections along STH 54 in the town of Hobart in Brown County on July 24, 2024, the inspection team found a constructed trench on private property on the south side of the detention pond embankment (SCP 373). The trench leads from a grease bin located on the private property. The potential spill was reported to the DNR Spills Hotline on 7/29/25 (WI SPILL #20852 ID 20240729NE05-1 - COOKING GREASE [FOOD PRODUCT]). No further information was received from the DNR.

2.3.6 Unauthorized Discharges

WisDOT is not aware of any unauthorized discharges in 2024.

2.3.7 Goals for Illicit Discharge Detection and Elimination Program

To meet the conditions of this section, the “Additional DOT Developed Measurable Goals” with tentative schedule is included in **Section L**.

2.4 Construction Site Pollutant Control Program

Best management practices are used to avoid or minimize soil, sediment, and pollutant movement from the project site to off-site areas, storm sewer inlets and waters of the state. The construction site pollutant program has multiple components:

1. Requirements as prescribed in Trans 401 and the TCGP.
2. Standardized erosion control details, specifications, and bid items.
3. Preparation of site-specific erosion control and restoration plans and special provisions included as part of the projects contract.
4. Design guidance provided in FDM chapter 10.
5. Construction guidance provided in CMM Chapter 645.
6. High level design and construction issues are discussed in the Cooperative Agreement.
7. Forms and processes have been developed to provide a consistent approach to meet requirements during the construction of projects.
8. On-sight construction staff to provide inspections and oversight at each project.
9. Regional Stormwater & Erosion Control Engineers (SWECEs) provide design reviews, ECIP reviews, spot check field reviews and are a technical resource for the design and construction staff.
10. DNR Transportation Liaisons review and provide comments on the design and construction aspects of a project.
11. BTS-ESS Engineers work on program level policy issues and are a technical resource for the regions.
List continues on the next page.
12. Regional SWECEs provide informal training to staff.
13. The BTS-ESS staff, with assistance from the regional SWECEs provided Formal erosion control training in 2024 as discussed in Section 2.1.1. This formal training was also provided in 2025 with plans to continue on an annual basis.

2.4.1 Submit Plans to DNR TL

The project's plans, including erosion control plans and special provisions that apply to construction site erosion control and other environmental concerns, are provided to the DNR TL for review and comment at the 30%, 60% and Draft PS&E (Plans, Special Provisions and Estimates, sometimes referred to as the 90% plans) review stages. Note that some projects and/or regions use a different nomenclature to describe the review schedule rather than 30, 60, Draft PS&E but the review periods for the plans generally follow a similar time frame (early design, mid design, near final design). Usually, erosion control elements on the plans will not be developed until 60% and Draft plan stage.

2.4.1.1 Transportation Construction General Permit (TCGP)

The department continues to obtain TCGP coverage from the DNR for any Department directed and supervised project site with one (1) acre or more of land disturbing construction activity, including TS4, non-TS4, connecting highways, local program projects and non-highway transportation facilities projects.

TCGP coverage is requested at the same time a project requests final concurrence from DNR.

Amended TCGP coverage is requested for the contractor's ECIP and any amendments to the ECIP, including major changes in project scheduling or the addition of temporary support sites that WisDOT agrees to cover under the TCGP. Amended coverage may also be requested for any significant changes to the project erosion control plans.

2.4.2 Facilities Development Manual Updating

The FDM provides policy, procedural requirements, and guidance encompassing the facilities development process within the Wisconsin Department of Transportation (WisDOT), Division of Transportation Systems Development (DTSD). It is applicable to all types of highway improvements on the state trunk highway system, other street/highway systems for which federal-aid highway funds may be utilized, state facilities road systems funded with state funds administered by WisDOT, and other highways and roads for which WisDOT may act as an administrative agent.

The first part of FDM Chapter 10 contains guidance on developing an erosion control plan and discusses various best management practices. The Department also addresses erosion control issues in the Construction Materials Manual (CMM). The Department reviews and updates the FDM and other guidance documents as necessary. A portion of the BTS-ESS Staffing Consultant's workload is to develop content for erosion control elements that are missing from the policy manuals and that need to be refined and updated.

2.4.2.1 Erosion Control Standard Specification and CMM Updates

Standard Specifications

The Department reviews and updates the Standard Specifications as necessary.

CMM Updates

The Department reviews and updates the CMM as necessary.

WisDOT provides the CMM to help staff administer and inspect construction projects. The goal is to provide uniform application and enforcement of contract requirements. The CMM clarifies the contract and may reference contract requirements but does not supersede the contract. The CMM also communicates department policies, practices, and expectations to consultants, contractors, and other construction industry partners. CMM guidance is based on common industry practice for transportation construction work. Situations not covered require experience, engineering judgment, and advice from experienced engineers, supervisors, or subject matter technical experts.

2.4.3 Erosion and Sediment Control Compliance Procedure

The department continues to follow the compliance procedures it has developed in the past and has refined over the years. The main documents used to provide a consistent approach are:

- The Erosion Control Implementation Plan (ECIP) form.
- The Erosion Control Inspection form.
- The Erosion Control Order form.

ECIP Form

While the Department provides as much detail in the plans and special provisions as possible during the design phase of projects to minimize environmental impacts, the details of the contractors means and methods are not known until the contract is awarded. As part of the contract, the Department requires the contractor to submit an Erosion Control Implementation Plan (ECIP) to address construction site sediment, erosion, and pollution control measures and site restoration. The department has a standardized form with instructions. The ECIP also covers additional impacts that the contractor may have for project related borrow, material disposal, and other temporary support activity sites.

An ECIP is required whenever a project has one or more of the following:

- Land disturbing construction activity.
- Plans include erosion control bid items.
- The contractor proposes to use temporary support activities for the project.
- There is a reasonable potential for pollutants to discharge into waters of the state.

ECIPs are reviewed by the regional SWECE, project construction oversight staff and the DNR TL. Land disturbing construction activity cannot start until the department approves the ECIP.

Erosion Control Inspections

Erosion control inspections, using the departments standardized Erosion Control Inspection Form, are required as follows:

- At least once per week.
- Within 24 hours after every precipitation event that produces 0.5 inches of rain or more during a 24 hour period.
- Following a discharge of sediment or pollutants from the project site into a water of the state.
- At the beginning of each new stage of construction.

- Upon completing the installation of permanent BMPs.
- At the completion of the project, when the site has met the final stabilization requirements.

The report must be provided to the project engineer immediately after the inspection is completed and is also posted at the construction field office. Project leaders are also encouraged to provide a copy of the inspection to the prime contractor.

Erosion Control Orders

The department has a standardized Erosion Control Order form that is used to direct the contractor to make necessary BMP repairs (attaching the Erosion Control Inspection form) and/or directing the installation of new BMPs based on the current conditions of the project. There are time frames for completing the work associated with the form and the department may assess monetary penalties for failure to perform the actions required in the order.

2.4.3.1 Erosion Control BMPs Repair and Replacement

As discussed above, when inspections identify needed repairs, deficiencies and additions the department attaches the inspection form to the erosion control order that directs the contractor to perform the work. The form requires signatures of both department and contractor staff to verify and document that the order is carried out within a timely manner. The signature process transfers the liability for the costs associated with a discharge of pollutants between the department and the contractor as described in Trans 401.

2.4.3.2 Amending Erosion Control Plans

Amendments to the Erosion Control Plans developed as part of the project's design and finalized prior to being let for contract are rare but would be thoroughly discussed with and reviewed by the DNR TL.

However, all elements of how the project's erosion control will be implemented is included in the Erosion Control Implementation Plan (ECIP) that is prepared by the contractor after they are awarded a contract. The ECIP is essentially an amendment to the Erosion Control Plan. When a project also has TCGP coverage, the DOT requests that the ECIP be an amendment to the TCGP coverage, when appropriate.

2.4.3.3 Non-compliance with Performance Standards

The Department also has specific authorities to ensure project compliance for construction site erosion and sediment control and reduce risks to the environment. Trans 401 and the provisions in the Standard Specifications provides the Department with legal authority to suspend or limit grading operations, when necessary, to prevent or minimize impacts to natural resources and ensure adequate performance of permanent erosion control measures. Additionally, the specifications provide the Department with latitude to assess penalties of \$500 per day for noncompliance with the erosion control orders.

2.4.3.4 Tracking Procedures

The department will be working on this requirement beginning in 2026 by investigating what other states are doing to track construction inspections and corrective actions.

2.4.4 Measurable Goals

To meet the conditions of this section, the “Additional DOT Developed Measurable Goals” with tentative schedule is included in **Section L**.

2.5 Post-Construction Stormwater Management Program

2.5.1 TCGP Conditions

The Department understands that WisDOT directed projects, including TS4, non-TS4, connecting highways and local program projects, with 1.0 acre or more of land disturbance shall be implemented in accordance with the post-construction related conditions in the WPDES General Permit No. WI – 066796 (TCGP).

2.5.2 FDM Ch. 10 Erosion Control and Stormwater Quality Updates

The Department is preparing significant updates to the Post-Construction Section of FDM 10. Some of the major elements of the update and their approximate percentages complete include:

1. Finalizing a Stormwater Management Plan (SWMP) form to assist designers in determining performance standards (75%)
2. Updating Section 10-25 and Section 10-40 (Stormwater Quality) to reflect the new preliminary and final design analysis and submittal processes. (50%)
3. Developing Stormwater Management Plans using preliminary and final design examples for each stormwater control practice. (Varies)
4. Adding more guidance on vegetated swales (85%)
5. Adding planning-level guidance for each stormwater control practice. (Varies)
6. Adding the following stormwater control practices:
 - a. Dry detention ponds (50%)
 - b. Infiltration/Biofiltration Stormwater Quality Design (25%)
 - c. Other Device Stormwater Control Practice Applications (75%)

A status update and completion schedule will be provided in next year’s report.

2.5.3 Drainage Area Modeling Procedure

2.5.3.1 Purpose

This section outlines a standardized procedure for capturing drainage areas associated with all projects initiated on or after December 18, 2023, that impact the DOT’s ROW. It includes provisions for integrating these drainage areas into WisDOT’s GIS mapping of the TS4 and for updating stormwater pollution prevention modeling—particularly in cases where hydrologic and hydraulic (H&H) calculations identify new sources of water quantity run-on from outside the ROW.

2.5.3.2 Capturing Drainage Areas

Refer to the Stormwater Program Guidance Manual Section 3 – Desktop and Field Data Collection for Stormwater Reporting Activities. Note that the ‘Stormwater Program Guidance Manual’ that was originally developed in 2018 is in the process of being expanded and refined for all the extra stormwater related activities and will be available on the Stormwater Management webpage when it is finalized.

2.5.3.3 Stormwater Pollution Prevention Modeling

Refer to the Stormwater Program Guidance Manual Section 4 – Stormwater Quality Modeling.

2.5.3.4 Integrating Drainage Area into WisDOT's GIS Mapping

Refer to the Stormwater Program Guidance Manual Section 5 – Integrating Modeling, Mapping and Annual SWPPP and SCP Inspections into Department's GIS mapping.

2.5.4 SCP Inventory

All the SCPs built by WisDOT are shown on the maps in **Section D** and listed on the spreadsheets in **Section H**. Local program and connecting highway SCPs are owned and maintained by the local jurisdiction. Ownership is noted on the spreadsheets. The maps only show SCPs owned by the Department.

2.5.4.1 Maintenance Program

WisDOT continued to make progress in 2024 on its statewide SCP inspection and maintenance program by performing more inspections and developing a process to perform necessary repairs and maintenance. The site-specific inspection and maintenance program for the I-39/90 Corridor from Beloit to Madison is active. There are also numerous agreements between WisDOT and municipalities to maintain selected stormwater control practices where runoff from the state facilities and municipality are combined. BTS-ESS is planning to compile and organize all the agreements to verify the party responsible for inspections and maintenance.

2.5.4.1.1 Inspection and Maintenance Guidelines and Inspection Forms

Inspection forms and maintenance guidelines have been developed for several common stormwater control practices including: wet detention ponds, dry detention ponds, infiltration basins, bioretention basins, infiltration trenches, vegetated filter strips, and enhanced grass swales. The guidelines include categories for inspection, urgent maintenance, routine maintenance, and non-routine maintenance. A copy of the inspection forms and associated guidelines are included in **Section F**. WisDOT will continue to review and refine the content of these guidelines and inspection forms as needed.

2.5.4.1.2 SCP Inspection and Maintenance Program

The Bureau of Technical Services at WisDOT finalized its SCP inspection and maintenance program in 2021 to inspect representative areas to determine the level of effort and costs associated with evaluating the condition of stormwater control practices. The SCP inspection program is ongoing and procedures to effectively facilitate maintenance when required have been developed. Inspections resulting in deficiencies requiring urgent maintenance follow the guidelines in Section 6 – Fieldwork and Desktop Protocols, in the Stormwater Program Guidance Manual.

2.5.4.1.3 SCP Inspections from 2021 to 2024

Building upon the 2021 SCP Pilot Inspections and the 69 SCPs inspected in 2022, the Department inspected 150 SCPs during 2023 and 153 SCPs in 2024. The locations of the inspections and any needed urgent repairs are broken down by inspection year and region in Tables 2.5.4.1.3.1 and 2.5.4.1.3.2 below:

Table 2.5.4.1.3.1 – SCP Inspections Completed by Year and Region

Region	Number of Inspections in 2021	Number of Inspections in 2022	Number of Inspections in 2023	Number of Inspections in 2024
NCR	0	13	0	7
NER	0	11	0	54
NWR	0	0	0	21
SER	49	0	150	26
SWR	0	45	0	45
Totals	49	69	150	153

Table 2.5.4.1.3.2 – Urgent Repairs Required by Year and Region

Region	Number of Urgent Repairs Needed in 2022	Number of Urgent Repairs Needed in 2023	Number of Urgent Repairs Needed in 2024
NCR	1	0	0
NER	0	0	8
NWR	0	0	4
SER	0	9	2
SWR	6	0	6
Totals	7	9	20

Information regarding completed SCP inspections and any urgent repairs noted during field inspections can be found in the SCP Database spreadsheet in **Section H** of this report. The database provides a count of Urgent Maintenance items at each SCP and notes on any issues found. The database also lists the latest inspection date for each SCP.

The inspection locations were determined by geographic location, with the goal of efficiently performing as many inspections as possible per year.

In 2025, more SCP inspections are planned with the goal of completing the first inspection of all statewide SCPs. The Department will also begin inspecting the sediment depths of WisDOT-owned wet detention ponds to determine when they will need to be cleaned or dredged.

2.5.4.1.4 Status of SCP Repairs/Maintenance

Urgent maintenance issues that may impact the roadway such as by severe erosion immediately adjacent to the travel lanes or potential flooding are forwarded to the regional Maintenance Engineering staff who then work with their local County DPW staff to correct those types of issues if possible.

Repairs and maintenance that are not urgent will be completed by the Regional Offices through contracts developed to complete the necessary repairs. The Department has not yet decided upon all the details of how those contracts will be established and funded.

In 2024, the SER completed the Construction Let project to repair some significant erosion related issues at some stormwater wet ponds in an urban freeway area known as the Zoo Interchange in Milwaukee County.

2.5.4.1.5 Tracking Inspections and Repairs/Maintenance

WisDOT has updated its “Stormwater and Environmental Map Viewer” (a GIS map) with locations of all its SCPs in 2024. This map also links to inspections that have been done beginning in 2021. As inspections are completed year by year, those results will also be included.

Inspections completed in 2024 will be added to the GIS mapping in 2025.

2.5.5 Goals for Post Construction Stormwater Management Program

To meet the conditions of this section, the “Additional DOT Developed Measurable Goals” with tentative schedule is included in **Section L**.

2.6 Pollution Prevention Program

2.6.1 DOT Highway Runoff Management

Details of the highway runoff management activities are listed in **Section A**. The TS4 Permit requires that WisDOT address the following highway runoff management activities:

2.6.1.1 Street Cleaning

The Department provides funding to counties to complete street cleaning programs. Currently WisDOT has not quantified the extent of lane miles or tons of material collected. BTS-ESS finalized a new consultant contract in 2025 to assist in developing a method to collect this information.

2.6.1.2 Catch Basin Cleaning

The Department provides funding to counties for catch basin cleaning on an annual basis. Currently WisDOT has not quantified the extent of material collected. BTS-ESS finalized a new consultant contract in 2025 to assist in developing a method to collect this information.

2.6.1.3 Collected Waste Materials

County DPW forces collect the waste from street sweeping and catch basin cleaning. They are expected to dispose of the waste according to procedures described in NR 528 – Management of Accumulated Sediment from Stormwater Management Structures.

2.6.1.4 Turf Management

All highway project designs specify vegetation planted in the ROW. Herbicide and pesticide treatments align with DATCP regulatory requirements and follow the application procedures detailed in the Department's highway maintenance manual for highway maintenance operations.

2.6.1.5 Winter Road Maintenance

The Department has developed a 10-part Salt Minimization Plan designed to reduce salt usage while maintaining effective roadway de-icing. **Section A** provides detailed information on each

component. Most of these components are also highlighted in WisDOT's 2023–2024 Annual Winter Maintenance Report, which is linked in the appendix.

The Department's consultant has started work on a contract to develop data collection and mapping standards for highway winter road maintenance activities.

2.6.2 Stormwater Pollution Prevention Plans (SWPPPs)

WisDOT has identified 45 facilities at 31 sites around the state that are not highway facilities but are owned and maintained by WisDOT. See the list of facilities in **Section J**. These facilities include storage yards, sign shops, electrical shops, design facilities, salt storage, laboratories, and garages. The Department has prepared SWPPPs for all these sites and has completed the second year of annual inspections for all sites in 2024. The Department is developing the process for quarterly wet weather inspection reporting utilizing consultant resources, which will begin in 2025.

2.6.3 Training

SWPPP training for facility managers and others interested or involved with the SWPPPs was provided jointly by the Statewide Stormwater Quality Engineer and the BTS stormwater consultants who prepared the SWPPPs on July 25, 2023. The approximately 1.5 hours training was done remotely with 49 attendees.

The consultant resources that provide the annual inspections walk each SWPPP site with the Facility Manager(s) and designees and provide an informal training, if requested.

The SWPPP training PowerPoint Slides and audio recording of the training are provided on the stormwater webpage for those that missed the training and for reference - [Wisconsin Department of Transportation Stormwater Management \(wisconsindot.gov\)](https://www.wisconsin.gov/transportation/stormwater).

2.6.4 Updates

No new facilities were added in 2024.

2.6.5 Measurable Goals

To meet the conditions of this section, the "Additional DOT Developed Measurable Goals" with tentative schedule is included in **Section L**.

2.7 Stormwater Quality Management

The storm water quality management program goal is to maintain compliance with the 20% reduction performance standard for total suspended solids in developed urban areas.

2.7.1 20% TSS Reduction Goal Status

The pollutant loading analysis to determine if WisDOT is meeting the 20% TSS reduction goal is complete. The results indicate that the particulate solids (TSS) yield reduction for all WisDOT facilities within the TS4 is 35%. These reductions vary by region and are shown in Table 2.7.1 below. The complete set of statewide reduction tables are in **Section G**. There is no statewide total phosphorus reduction goal; the total phosphorus percent reduction values are included for informational purposes. The Department's pollutant loading analysis process is included in **Section B**.

Table 2.7.1 – Statewide Runoff Volume and Pollutant Percent Reduction Summary

Region	Miles Mapped	Total Highway Source Area (ac)	Percent Reduction		
			Runoff Volume (%)	Particulate Solids Yield (%)	Total Phosphorus (%)
NE Total	312.5	10,078	24%	35%	29%
NC Total	58.7	2,098	41%	49%	41%
NW Total	122.9	5,092	57%	64%	63%
SE Total	533.0	14,704	17%	25%	22%
SW Total	215.5	10,932	33%	41%	34%
Statewide Total	1242.6	42,905	26%	35%	31%

2.7.2 Retrofit Stormwater Control Practices

Some existing stormwater control practices put in place by WisDOT may be improved to meet the TSS reduction goal. Potential improvement strategies are listed below by stormwater control practice type.

Detention ponds- Converting a dry detention basin to a wet detention pond or an extended dry detention pond improves settling capacity and can reduce TSS. Basin factors to be evaluated before this conversion are drainage area characteristics, depth to groundwater, soil type, overall pond depth, airport proximity, and possible outlet structure modifications.

The Department will make use of the DNR’s proposed Vegetated Dry Detention Basin technical standard being developed through the SOC process.

Paved areas- The introduction of green infrastructure in paved areas such as park and rides or rest areas disconnects impervious material and improves infiltration.

Swales- Reseeding swales with native vegetation can deepen root structure and allow for increased infiltration. Widening swales and regrading to prevent clogging swales with sediment are other ways to improve swale capacity and reduce TSS.

Other Agencies Stormwater Proposals - The Department is open to other agencies’ proposals to use DOT ROW to install SCPs to provide extra stormwater treatment for both DOT runoff and off-site runoff if the project can be done without negative impacts to the roadway and any planned projects. For example, as reported in the 2023 Annual Report, the Department allowed (by Work in ROW permit) the Milwaukee Metropolitan Sewerage District (MMSD) and City of Milwaukee to install Green Infrastructure that included a treatment train of SCPs under a portion of the elevated Marquette Interchange in downtown Milwaukee. The SCPs included end of pipe sumps, rock-lined swales, bioretention basins and porous pavers. DOT staff were very involved with the review and approval of these facilities. Table 2.7.2 summarizes the stormwater projects that have been coordinated with other agencies. Note that the overall statewide TSS reduction does not include the effect of these SCPs.

Table 2.7.2 – Stormwater Projects Coordinated with Other Agencies

Region	Other Agency(s)	Location	Stormwater Retrofit Project	Status
SER	Sponsor = MMSD Maintenance = City of Milwaukee	I-794, (under Marquette IC) e/of I-43 to w/ of S. 6 th St.	Green Infrastructure (GI)	Constructed
SER	Sponsor = MMSD Maintenance = City of Milwaukee	I-94 (Mineral Street Overpass) n/of W. National Ave. to s/of W. Virginia St.	GI	Constructed
SER	Sponsor = MMSD Maintenance = City of Milwaukee	I-94 (Becher Street Overpass) n/of W. Becher St. to s/of W. Rogers St.	GI	Constructed
SER	Sponsor = MMSD Maintenance = City of Milwaukee	I-794 (Port of Milwaukee) n/of E. Conway St. to E. Lincoln Ave.	GI	Proposed 2025 construction
SER	Sponsor = MMSD Maintenance = City of Milwaukee	I-94 (Holt Overpass) n/of W. Holt Ave. to s/ W. Oklahoma Ave.	GI	Design
NCR	Sponsor & Maintenance = Weston, Rothschild, Schofield. Supported by DNR grant	STH 29 and US 51 IC, NE loop ramp	Wet Pond	Planning

Note: Completed SCP/GI Plans have been added to the overall SCP database.

2.8 Storm Sewer System Maps

Section D contains statewide TS4 mapping organized by transportation region. These maps identify “High Risk” outfall locations and SCP sites throughout Wisconsin. As of 2024, this mapping is available to WisDOT staff via an online portal, which also displays inspection status for SCP sites and provides access to related inspection and maintenance documentation.

Detailed drainage maps for the 2010 urbanized area are provided in **Section E** of the 2023 TS4 Permit Report. Modeling efforts are underway for the additional highway extents covered by the 2020 urban areas, which will be added to the DOT’s internal GIS mapping system when completed.

WisDOT is currently conducting a pilot study to assess the level of effort required to identify all drainage areas flowing directly to the DOTs ROW and incorporate them into the DOTs GIS mapping system. The pilot study focuses on 2020 urbanized areas in Madison, Green Bay, and the City of Portage, and includes delineation of all drainage areas, storm sewer systems, SCPs, and outfalls as required in the TS4 permit.

Initial study findings indicate a significant resource investment for statewide mapping, with manual delineation efforts estimated conservatively at 16 hours per lane mile. This estimate

includes time for storm sewer data import from as-built plans, drainage area delineation, and identification of SCPs and outfalls. To improve efficiency, an automated script for drainage basin delineation and outfall identification is currently being developed using ArcPython.

A comprehensive cost estimate and implementation schedule for statewide mapping will be developed upon completion of the pilot study.

2.9 Annual Report

The intent of this Report is to facilitate the submittal through the DNR’s electronic system of the status of the TS4 conditions. The department has provided an opportunity through its Stormwater Management webpage for the public to comment on the Annual Report.

2.9.1 Status of 6 Stormwater Program Areas

The status of the six stormwater programs has been discussed in this report.

2.9.1.1 Summary of Inspections

Inspection results are discussed in this report.

2.9.1.2 Program Effectiveness

The department has made significant progress towards meeting the conditions in the TS4 Permit and in improving its stormwater program overall as discussed in this report.

2.9.1.3 Research Projects

The department was not involved in stormwater related research in 2024.

Table 2.9.1.3 provides a summary of the 2025 stormwater research initiatives in which the Statewide Stormwater Quality Engineer will be actively engaged.

Table 2.9.1.3 – Stormwater Research Initiatives

Project	Sponsor	Lead Investigator	WisDOT Role & Status
NCHRP 25-76: Design Guidelines for Traversable Roadside Stormwater Check Dams in Clear Zone	NCHRP	TBD	Panel Member – approved to be a panel member
Development of Biochar Specification Criteria as Soil Amendment for Slopes, Conveyances and Stormwater Treatment Systems, Phase 2	Minnesota DOT	Natural Resources Research Institute, University of Minnesota Duluth	Panel Member – Project kick off July 1, 2025

2.9.1.4 Fiscal Analysis

Table 2.9.1.4 below lists total wages for WisDOT regional and central office staff who spend the majority of their time working on stormwater and erosion control issues and for consultants hired by BTS-ESS to provide support.

Table 2.9.1.4 – Fiscal Analysis

Funding Category	Amount Paid in 2024	Notes
Regional SWECE Staff	\$937,857.26	For 8.5 FTE's
Central Office Stormwater and Erosion Control Engineers	\$210,447.88	For 2 FTE's
Consultant Resources	\$543,025.63	For 2 consulting firms to provide support for mapping, TS4 reporting, field work, TMDL related work, modeling, policy manual updating and other technical support as needed.

Note: This table does not include costs for regional project design or construction oversight.

2.9.2 Other Entities

The department contracts with Wisconsin counties to perform highway maintenance activities, including mowing, drainage maintenance, street cleaning, and winter snow and ice control.

2.9.3 Updated Mapping

Detailed drainage maps for the 2010 urbanized area are provided in Section E of the 2023 TS4 Permit Report. Modeling efforts are underway for the additional highway extents covered by the 2020 urban areas, and they will be added to the DOT's internal GIS mapping system when completed.

2.9.4 Water Quality Degradation

The department is not aware of any water quality degradation in the TS4.

2.9.5 Summary of TMDL Actions

The department's consultant is working on the mapping and modeling of the TMDL watershed, but this task is not completed. Refer to Section 3.0 for additional information.

2.9.6 Signer

The department's Director of the Bureau of Technical Services will sign the report.

2.10 Reapplication for Permit Coverage

The department understands that it needs to apply for permit coverage renewal by June 16, 2028, and that the application shall include the items listed in this section of the current TS4 permit.

2.11 Amendments

The department is not currently requesting any amendments.

3.0 TMDL Requirements

3.1 Tabular Summary of TMDL Reachshed

The department's consultant is working on the mapping and modeling of the TMDL watershed, but this task is not completed. TMDL mapping will be updated by the end of 2025, after which all available pollutant load calculations from within each TMDL reachshed will be tabulated and included in the following annual report.

3.2 List of Projects Expected to Require TCGP Coverage Within the TS4

This task will be started upon completion of the mapping and modeling of the TMDL watershed described in Section 3.1 of this report.

3.3 Anticipated Progress Towards TMDL Wasteload Allocation

This task will be started upon completion of the mapping and modeling of the TMDL watershed described in Section 3.1 of this report.

3.4 Updated Summary of Pollutant Reduction for Each TMDL

This task will be started upon completion of the TMDL wasteload allocation for project work described in Section 3.3 of this report.

3.5 TMDL Compliance and Implementation for Bacteria WLAs

3.5.1 Milwaukee River TMDL Bacteria Inventory and Map

The department made significant progress towards this task in 2024 by its initial Pilot Study of a portion of the watershed. A copy of the report is included in **Section K**. A link to the report is on the stormwater webpage - [Wisconsin Department of Transportation Stormwater Management \(wisconsindot.gov\)](https://www.wisconsin.gov/stormwater)

The rest of the watershed is being worked on, and it is anticipated to be completed in 2025.

3.5.2 Collaboration with Adjacent Municipalities.

If bacteria sources are found as a result of this task, WisDOT will work with the adjacent municipalities to eliminate the issues. No bacteria sources were found in the Pilot Study.

4.0 Implementation Schedule

PERMIT SECTION	ACTIVITY	IMPLEMENTATION DATE	DOT or ORIGINAL DNR COMMENTS
1.5.1	Discharges to an impaired water body	September 30, 2025, and by September 30 of each odd-numbered year thereafter	DOT Comment: Consultant contract for this work will be developed in 2025.
2.1 to 2.6	Submit updated written programs including measurable goals	September 30, 2024	DOT Comment: Measurable goals included in 2024 TS4 Report.
2.3	Illicit Discharge Detection and Elimination – Dry weather screening of high-risk outfalls	By September 30 in annual report	DOT Comment: No illicit discharges detected from high-risk outfalls in 2024.
2.3	Illicit Discharge Detection and Elimination – Train maintenance and inspection staff on illicit discharge detection and elimination procedures	Minimum of once per permit term or when new staff are completing requirements. Include record in September 30 annual report of the year it takes place.	DOT Comment: Training occurred with WisDOT staff in 2023.
2.6	Pollution Prevention – storm water quality management section 2.6.1.1, implement storm water management program	September 30, 2027	DOT Comment: Section 2.6.1.1. references street cleaning. WisDOT Implemented highway maintenance consultant contract in 2/2025 to document street cleaning practices.
2.6	Pollution Prevention – storm water quality management section 2.6.1.2, submit evaluation of flood control structures	September 30, 2025	DOT Comment: Implemented highway maintenance consultant contract in 2/2025 to document catchbasin cleaning practices.
2.6	Pollution Prevention – storm water quality management section 2.6.1.3, submit assessment of compliance	September 30, 2025	DOT Comment: Implemented highway maintenance consultant contract in 2/2025 to document catchbasin cleaning practices including collected waste disposal.
2.6	Pollution Prevention – section 2.6.2, complete a highway runoff management program	Plan by September 30, 2024. Implement by September 30, 2025.	DOT Comment: 1) Annual SWPPP inspection took place in fall of 2024. 2) Wet weather quarterly SWPPP inspections to begin in fall of 2025.
2.6	Pollution Prevention – section 2.6.3, complete an inventory of DOT owned or operated storm water management control practices	Update annually with September 30 annual report	DOT Comment: SWPPP Inspection training took place for WisDOT staff in July, 2023.

2024 TS4 Permit Report

2.6	Pollution Prevention – section 2.6.4, complete a schedule of routine inspection and maintenance of DOT owned or operated storm water management control practices	Report on progress annually, with September 30 annual report	DOT Comment: No new SWPPP facilities.
2.6	Pollution Prevention – section 2.6.5, complete SWPPPs for DOT facilities	September 30, 2023	DOT Comment: Section 2.6.5 is measurable goals. All WisDOT SWPPPs had their first inspection in 2023.
2.6	Pollution Prevention – section 2.6.6, training modules for DOT staff	September 30, 2024	DOT Comment: There is no section 2.6.6, this information is referenced in Section 2.6.3.
2.7.1	If DOT is currently not achieving at least a 20 percent reduction, DOT must prepare and submit its plan to the Department within 12 months of the permit start date.	Within 12 months of the permit issuance date	DOT Comment: As shown in Table 2.7.1, all regions have achieved at least a 20% TSS reduction.
2.8	TS4 Map – submit TS4 map	Objective of completing the statewide system by end of the permit term.	DOT Comment: TS4 area mapping is proceeding through a consultant contract.
2.8.1.1.1	TS4 Map – Drainage Basins and storm sewer systems	<ol style="list-style-type: none"> 1. Complete a portion of drainage basin and storm sewer mapping each year with the goal of completing 20% annually and the requirement of completing the statewide system by end of the permit term. 2. By September 30, 2024, submit a schedule identifying areas to be mapped each year. 	<p>DNR Comments:</p> <ol style="list-style-type: none"> 1. DOT coordination and criteria development for overall functionality of GIS being developed. 2. 2025: Desktop storm sewer system mapping and drainage basin delineation of representative areas (Madison, Green Bay & Portage). 3. 2026: Ground truth desktop data, finalize pilot and analyze costs. 4. Determine feasible yearly on-going mapping using pilot program costs. DOT to provide status update as part of Annual Report. <p>DOT Comment: Drainage basin mapping is proceeding through a consultant contract</p>

2024 TS4 Permit Report

2.8.1.2	TS4 Map – Up to Date Urban Area for TMDLs	<p>Update the 2020 census results by no later than 1 year after publication of census maps. This will be submitted with the annual report on September 30 one year after the publication of the census maps.</p> <p>Update TS4 permitted boundaries as needed based on the 2020 data. Provide update biannually (odd-numbered years) as updates are made.</p>	DOT Comment: TMDL mapping is proceeding through a consultant contract.
2.8.1.3	TS4 Map – Outfall Locations	<p>At least 20% of minor outfalls to be mapped annually with the goal of completing all the minor outfalls by the end of the permit term.</p> <p>Update major outfalls as changes are made to the system to be submitted biannually on odd-numbered years with the annual report on September 30th.</p>	DOT Comment: Major and minor outfall locations associated with new construction or increased TS4 areas when compared to 2010 urban areas will begin upon completion of the TS4 maps.
2.8.1.4	TS4 Map – Discharges to TS4 from sites with WPDES permit	<p>Map locations of all other WPDES permittees draining to the TS4 by September 30, 2026. Update as needed thereafter to be submitted biannually on odd-numbered years with the annual report on September 30th.</p>	DNR Comment: DOT to request any available information from DNR.
2.8.1.5	TS4 Map – Major BMPs	Bi-annual updates as needed on the odd-numbered years with the annual report on September 30th.	DOT Comment: Stormwater Control Practices (SCPs) are added to the DOT SCP database annually. The current database is included in this report.
2.8.1.6	TS4 Map – DOT ROW or other DOT owned lands	Bi-annual updates as needed on the odd years to be submitted biannually on odd-numbered years with the annual report on September 30th.	DOT Comment: Approximate DOT ROW lines are added to DOT stormwater maps as they are produced. Final lines will be drawn when DOT mapping systems incorporate ROW documentation.

2024 TS4 Permit Report

2.8.1.7	TS4 Map – Location of SWPPPs	September 30, 2026 Bi-annual updates as needed on the odd years to be submitted biannually on odd-numbered years with the annual report on September 30th.	DOT Comment: All SWPPP locations are included in the current report maps.
2.8.1.8	TS4 Map – TMDL boundaries	1 year after the publication of census maps This will be submitted with the annual report on September 30 one year after the publication of the census maps.	DOT Comment: TMDL boundaries will be incorporated in the maps developed through a consultant contract in 2025.
2.8.2	TS4 Map – Overall Updating	Bi-annual updates as needed on the odd years to be submitted biannually on odd-numbered years with the annual report on September 30.	DOT Comment: The DOT currently restricts access to the stormwater mapping system to DOT employees only. It is not known if/when this policy will change.
2.9	Annual Report	September 30 of each year, reporting on the previous calendar year	DOT Comment: Submitted as required.
2.10	Reapplication	180 days prior to the expiration date	DOT Comment: Not addressed yet.
3.2	Submit list of planned projects expected to require coverage under the TCGP by TMDL and expected progress toward TMDL WLAs	September 30, 2025	DOT Comment: The list of projects will be compiled when the TMDL mapping and modeling work is completed.
3.1	Submit an updated tabular summary showing progress toward TMDL reductions	September 30, 2027	DOT Comment: The tabular summary should be complete by the implementation date.
3.3	Submit list of planned projects expected to require coverage under the TCGP by TMDL and expected progress toward TMDL WLAs	May 31, 2027	DOT Comment: The list of planned projects will be developed after the Department determines the current progress toward TMDL WLAs.
3.5.1	Submit the bacteria inventory map for the Milwaukee TMDL area.	September 30, 2026	DOT Comment: A consultant contract will be completed in 2026 to report on the bacteria inventory map.
3.5.2	Bacteria Source Elimination Plan for the Milwaukee TMDL area.	September 30, 2027	DOT Comment: If any bacteria sources are found from the bacteria inventory for the Milwaukee River TMDL, the Department will work with the DNR to develop a Bacteria Source Elimination Plan.

2024 TS4 Permit Report

5.0 Signatory Page

This annual report describing the compliance efforts of the Department to meet the requirements of the Transportation Separate Storm Sewer System Permit No. WI – S066800-1 has been reviewed and approved by the WisDOT Bureau of Technical Services Director.

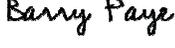
Signed by:

6C7F09D97FAB45D...
Prepared by

Hans Hallanger, PE

WisDOT Stormwater Quality Engineer

30 September 2025

DocuSigned by:

3F13959827A84D7...
Approved by

Barry Paye, PE

WisDOT Bureau of Technical Services Director

30 September 2025

Section A – Highway Runoff Management Activities

Section A - Highway Runoff Management Activities

The total costs associated with highway maintenance in 2024 are provided in this section.

Street Sweeping

The Department defines pavement sweeping as the removal and disposal of debris from pavements or paved shoulders with a street sweeper, by mechanical means or by hand. These activities happen in the spring as well as during the balance of the year (during the snow-free period). In addition to regularly scheduled sweeping, specific occurrences that could trigger a sweeping action include the removal of crash debris or spill debris removal.

In 2024, WisDOT paid Wisconsin counties a total of \$2.79 million dollars to sweep WisDOT highways. The amounts paid to each county ranged from \$324 for Clark County to \$1,052,542 which was to Milwaukee County to pay for regularly scheduled highway sweeping. Some counties do not have any sweeping resources, and so were not participants in this program.

Salt Use Minimization and Storage

The Department's Sensible Salting Program has a number of components that are intended to encourage counties to lower their use of all materials used to de-ice roads while maintaining the high level of safety that Wisconsin drivers have come to expect. WisDOT also believes that mandating particular processes, procedures, or equipment for all counties is not an effective way of promoting the program because of the variety of climatic conditions and operational systems throughout counties in the state.

The salt use reduction plan has ten components, each of which is discussed below.

- 1) **Salt storage requirements and site inspections.** Administrative code TRANS 277 requires that WisDOT annually inspect all salt storage sheds, sand/salt piles, and liquid chloride tanks that contain materials that are intended for winter maintenance on public roadways. In 2024, WisDOT inspected 1,306 locations containing 2,572 storage units. When a facility is out of compliance with the code, a letter is sent to the facility owner detailing what needs to be done to bring the facility back into compliance. In 2024, WisDOT returned from a self-inspection process back to a consultant led effort to complete inspections and 547 non-compliance letters were sent.
- 2) **Salt application rate guidelines.** The Maintenance Manual (Chapter 6-20-25) indicates that the salt application rates are normally between 100 and 400 lb/lane-mile, depending upon the temperature. The application rate table is attached with this Appendix.
- 3) **Pre-wetting guidelines.** WisDOT has encouraged counties to lower the salt application rate by pre-wetting the salt, which can cut the application rate by 50 to 100 lbs/lane-mile. Pre-wetting helps the mixture stick to the road instead of blowing off to the shoulder, which reduces the amount of material needed. It also helps the salt start working more quickly. Most counties are also using on-board pre-wetting units that carry a liquid pre-wetting agent (CaCl, MgCl₂, salt brine or an agricultural-based product) on the truck and spray the pre-wetter at the spinner and all counties in 2023 used pre-wetted salt at some point during the winter.

4) **Direct Liquid Application.**

The use of Direct Liquid Application (DLA) is relatively new in Wisconsin. Liquid brine, a simple mixture of salt and water, is applied directly to the pavement for deicing to replace rock salt as the primary storm management tool. This approach not only reduces the amount of salt applied but has been found to be more effective than solid salt applications. In an effort to support the implementation of this practice in Wisconsin, WisDOT funded the purchase of high-capacity brine makers (HCBM) for the counties, starting from the 2018-2019 winter season. Every year since then, WisDOT continued funding the purchase of HCBMs in more counties. As of the end of June 2024, WisDOT has funded a total of 53 HCBMs and looks to continue this effort as two additional counties are seeking to get funding for HCBMs in Fiscal Year 2025.

WisDOT has sponsored multiple research studies to learn more about DLA and its best practices and benefits. WisDOT contracted with UW TOPS (Traffic Operations and Safety) Lab to conduct a two-phase analysis of DLA technique in collaboration with the counties. The final report of phase 2 of this project was completed at the end of 2021, which builds on data collected from the counties in the 2020-21 winter season. This project analyzed the costs and benefits of DLA, and measured the salt use reduction, difference in achieving time to bare/wet, and friction rating of pavement when using liquids compared to granular salt. Statewide, 10 counties supplied data to UW TOPS Lab for this project. Some of the highlights from this project are:

- Benefits of DLA far outweigh the infrastructure costs to transition to a more liquid application model for winter highway maintenance events. Looking at only materials (salt) savings, over the winter 2020-21 period, DLA saves WisDOT \$5.95 per lane mile. With over 35 thousand miles in the state highway system and an average of 35 events each year, this is a significant saving.
- Comparing the use of DLA vs. granular salt, the counties participated in the study are shown as successfully reduced overall salt use by 23%, with data showing that during certain storms, salt use was reduced by well over 50%.
- Analysis of friction rating and time to bare/wet pavement comparing DLA use vs. granular salt showed that condition rating of pavement surface was on average of 15% better with DLA, and time to bare/wet pavement was reached 11.9% quicker.

WisDOT also championed a research study focusing on DLA through Clear Roads, a national research consortium focused on rigorous testing of winter maintenance materials, equipment, and methods for use by highway maintenance crews. This study is also done by UW TOPS Lab and completed in December 2021. It focuses on expanding the application rate guidance for liquid application. The result of this study is a comprehensive guidance on best practices of application rates of DLA, more specifically focusing on lower temperature DLA application rates.

- 5) **Ground speed controllers.** All counties use ground speed controllers. These controllers coordinate the spinner rate with the speed of the truck, so if the truck stops, the spinner stops. Ground speed controllers have proven through many studies to help keep salt on the roadway and reduce salt usage.
- 6) **Anti-icing techniques.** Anti-icing is a pro-active practice that places liquid material on roads or bridge decks before the precipitation event occurs. This practice requires less material than would be needed if salting occurs during or after a precipitation event. This practice effectively reduces the amount of material applied to the roadway. Efforts have also been made to use anti-icing techniques to get proactive applicator trucks out

sooner to apply the materials on the road sooner, resulting in less material being used. Sixty-nine of seventy-two counties (96%) have anti-icing equipment and made anti-icing applications in the 2023-2024 winter. Many counties also use alternative liquid chemicals, including magnesium chloride, that can lower salt application rates and put fewer chlorides in the environment. The standard anti-icing rate application table is attached with this Appendix.

- 7) **Infrared pavement temperature sensors.** The Department has encouraged the installation of pavement temperature sensors on salting trucks, so the driver gets a better idea when to place material. When the pavement temperature is over 34 degrees F or less than 15 degrees F, no application is needed. Between 15-23 degrees F, pre-wetting is recommended. More than 1,000 sensors are in use in the state, and they are used in every county. WisDOT also has 75 weather and pavement condition sensors along state highways to provide counties with information on current road conditions.
- 8) **Maintenance Decision Support System (MDSS).** MDSS is a program that takes information from the trucks such as spread rate, pavement temperature, speed, blade up/down, and combines it with weather data and other information to determine the appropriate pavement treatment during a winter storm. The program is designed to only apply the minimum amount of materials necessary to make the snow or ice slushy so that it can be mechanically removed. Other states have implemented this technology and have shown millions of dollars in salt savings. The program began in Wisconsin in 2009. The second phase of deployment, which included adding four or five “representative routes” in each county occurred 2010-2011. The remainder of the routes were input into the State’s MDSS system in 2011.
- 9) **TowPlow.** Two TowPlows were purchased in 2009 as part of a two-year study. These units consist of a trailer with a plow attached that can be pulled behind a larger more powerful snowplow (at least 350 hp engine). Together they can plow up to a 24 foot wide section in one pass thus reducing the need for an additional driver and reducing the need for salt. The study findings suggest that when compared to regular plows, TowPlow can result in operational savings (labor and fuel) between 32 to 43 percent. Based on the storm report database for the last 10 years, TowPlow could have been used by a sample county for 270 hours in a year on average. The resultant cost savings are estimated to be \$14,500 per year, and the TowPlow could pay for itself in five years. For counties with greater snowfall, savings per year would be greater and cost recovery time shorter. Currently only two counties are using TowPlows in the state, Brown and Dane counties. Both of them have ability to hold both salt and salt brine for de-icing on the tow plow.
- 10) **Underbody Plows/Blades.** Underbody blades are powerful snowplow blades that are mounted underneath a snowplow between the front and the rear axles. They use the weight of the truck to apply downward pressure that is sufficient enough to peel off ice and snowpack without the use of salt to burn it off. Currently there are 832 underbody plows in use in 60 counties.

Wisconsin participates in Clear Roads multi-state pooled fund studies designed to reduce salt usage. The pooled fund project, of which Wisconsin is a member, focuses on rigorous testing of winter maintenance materials, equipment, and methods for use by highway maintenance crews. Launched in 2004, Clear Roads now has 39 member states and has completed 98 research projects as of 2024. The following link is to the Clear Roads webpage containing information about all of the previous and ongoing research projects: [All Research and Synthesis Projects - Clear Roads](#). The Clear Roads projects completed in 2024 are briefly discussed below.

- a) **Calculating Plow Cycle Times from AVL Data.** The goal of this project was to develop a methodology to calculate plow cycle times, considering various relevant factors. This methodology can be used to create the framework for a visualization tool that agencies can format with their own electronic data.
- b) **Using GIS to Highlight Highway Segments Sensitive to Deicing Materials.** The goal of this project was to improve operational planning to help agencies identify the roadway segments where vulnerable environmental resources may be impacted by snow and ice control activities.
- c) **Standard Test Procedures for Ice Melting Capacity of Deicers.** The goal of this project was to provide transportation agencies with an objective, repeatable test procedure for measuring the ice melting capacity of deicers.
- d) **Training Module Development for Evaluation of Storm Severity Index and Winter Severity Index Variables.** The goal of this dissemination and implementation project was to develop Clear Roads training modules for three general audiences, including division directors, snow and ice managers, and supervisors.
- e) **Update to CR 13-04: Best Practices for Protecting DOT Equipment from the Corrosion Effect of Chemical Deicers.** The goal of this project was to update and expand Chapter 5.4 of the Best Practices Manual for the Clear Roads Project entitled, CR 13-04 Best Practices for the Prevention of Corrosion to DOT Equipment: A User's Manual, by looking at the various coatings available on the market, from 2013 to the present, and conduct side-by-side comparison testing based on the application method.
- f) **Salt Shed Design Template.** The goal of this project was to develop a set of three scalable salt shed designs that can be used as a template to meet the needs of a variety of sites throughout Clear Roads member states. The designs were intended to take into account existing building codes and state and federal regulations associated with environmental and related concerns.
- g) **Best Management Practices for Liquid Chloride Storage and Pumping Systems.** The goal of this project was to document and share optimal system designs and management practices that contribute to safe, sustainable, and cost-effective liquid storage systems.
- h) **Use of Dashboards for Winter Operations.** The goal of this synthesis project was to provide state winter maintenance managers with an understanding of how dashboards are being used to track and manage winter maintenance operations.

Catch Basin Cleaning

The Department includes catch basin cleaning as part of the “cleaning and repairing drainage structures” item, which is defined as “Cleaning and maintaining culverts, catch basins, inlets, storm sewers, flumes, riprap, and other drainage structures, including the replacement of failed culverts.” The most recent cost data shows that the total cost for all these cleaning and maintenance activities billed to the state by the counties in 2024 was \$5.44 million dollars.

No further cost breakdown exists, although it is likely that catch basin cleaning accounts for a very small fraction of the total amount because WisDOT practice has been to design catch basins without sumps. In addition, any new construction project that includes sumps should also include an agreement with the local municipality to clean the sumps.

Herbicide Application Guidelines

The Department has prepared updated guidelines in the Highway Maintenance Manual (HMM 07-05-15 and 07-10-05) for the use and application of herbicides for herbaceous and woody vegetation adjacent to highways. The section includes general application guidelines, safety and spill guidelines, types of herbicides, using herbicides to control vegetation on highway shoulders and around guardrails and a discussion of agreements required for no-spray areas. It also addresses permit requirements for applying an herbicide to a water of the state.



Anti-Icing Guidelines					
PREDICTED PRECIPITATION EVENT	Recommended Locations	Application		Rate	COMMENTS
		Liquid (gal/lane-mi.)	Pre-wetted Salt (lb/lane-mi)		
Frost or Black Ice	Bridge Decks and Trouble Spots	20-30 (frost) 30-40 (Black Ice)		50-150	1) Consider treating approaches as well as bridge decks. 2) Treat ice patches, if needed, with pre-wetted salt at 100 lb/lane-mi.
Sleet	Bridge Decks and Trouble Spots and Intersections	20 Recommended 30 Maximum		200-400(1) 100-300(2)	1) Consider treating approaches as well as bridge decks. 2) Treat ice patches, if needed, with pre-wetted salt at 100 lb/lane-mi.
Freezing Rain	Any area of concern	Not Recommended		200-400(1) 100-300(2)	It is not recommended to apply liquid de-icing agents in an anti-icing mode prior to freezing rain events.
Light Snow (< 1/2" in./hr.)	Trouble Spots and Intersections	30 Recommended 40 Maximum		100-200	If anti-icing is performed prior to a snow event, re-application may be necessary to prevent re-freeze. It also may be necessary to switch to a de-icing mode.
Moderate or Heavy Snow (≥ 1/2 in./hr)	Trouble Spots and Intersections	40 Recommended 50 Maximum		100-300	1) Do not apply liquid anti-icing agents onto heavy snow accumulation or packed snow. 2) Applications will need to be more frequent at lower temperatures and higher snowfall rates. 3) If anti-icing is performed prior to a snow event, re-application may be necessary to prevent re-freeze. It also may be necessary to switch to a de-icing mode.
Notes:					(1) 4-Lanes and Greater (2) 2 Lanes

- Anti-icing operations typically should be conducted during normal, non-overnight working hours and low traffic volume periods.
- It is not recommended to apply de-icing agents in an anti-icing mode when the pavement temperature is below 15°F or drifting is a problem.
- Time initial anti-icing agent applications and subsequent de-icing agent applications to prevent deteriorating conditions or development of packed and bonded snow.

Standard Anti-Icing Rate Application Table

De-icing Application Rates for Pre-Wetted Salt – (Two-Lanes)

This guide is not meant to be a substitute for the use of judgment and the observation of the result of treatments on existing conditions. It is meant to show variables that usually occur together and the treatment that has proven to be the most successful. This guide should then be used to assist in deciding on the best course of action depending on existing conditions. This table assumes the salt is pre-wetted. (Allow de-icing agents time to begin working before making additional plowing passes.)

2-lane Highways Application Guidelines #/LM Pre-wetted Salt	Pave. Temp. 28° to 32° F		Pave. Temp. 23° to 28° F		Pave. Temp. 15° to 23° F		Pave. Temp. Less than 15° F	
	Initial	Subsequent	Initial	Subsequent	Initial	Subsequent	Initial	Subsequent
Frost	100	50-100	100-150	50-150	100-200 ²	100-150 ¹	100-300 ^{1,2}	100-200 ^{1,2}
Black Ice	200	100-200	100-300	100-200	100-300 ²	100-300 ¹	100-300 ^{1,2}	100-300 ^{1,2}
Sleet/Freezing Drizzle	200	100-200	100-300	100-200	100-300 ²	100-200 ¹	100-300 ^{1,2}	100-300 ^{1,2}
Freezing Rain	100-300	100-200	100-300	100-200	100-300 ²	100-300 ¹	200-300 ^{1,2}	100-300 ^{1,2}
Dry Snow	100-200	100-200	100-300	100-200	Plow Only ¹	Plow Only ¹	Plow Only ¹	Plow Only ¹
Wet Snow	200	100-200	100-300	100-200	100-300 ²	100-200 ¹	100-300 ^{1,2}	100-300 ^{1,2}

- Mechanical means of snow removal is the preferred method. Before applying any de-icing agents, the surface should be cleared of as much snow and ice as possible by mechanical means.
- Application rates are "MAXIMUM RECOMMENDED RATES". Only apply the amount of pre-wetted salt necessary to accomplish the desired level of service. Rates may vary with regard to pavement temperature, type of roadway surface, and weather conditions.
- Abrasives should not be used on roadways where speeds in the sanded areas exceed 45 mph.
- When wind speed is over 15 mph, use caution when salting and applying moisture drawing de-icing agents.
- ¹ Intersections and low speed hazardous areas may be treated with pre-wetted abrasives when warranted.
- ² If necessary, use alternate de-icing agents like calcium chloride and magnesium chloride in combination with a lower application rate of salt.

11/08

De-icing Application Rates for Pre-Wetted Salt (Four Lanes and Greater)

This guide is not meant to be a substitute for the use of judgment and the observation of the result of treatments on existing conditions. It is meant to show variables that usually occur together and the treatment that has proven to be the most successful. This guide should then be used to assist in deciding on the best course of action depending on existing conditions. This table assumes the salt is pre-wetted. (Allow de-icing agents time to begin working before making additional plowing passes.)

4-lane Highways Application Guidelines #/LM Pre-wetted Salt	Pave. Temp. 28° to 32° F		Pave. Temp. 23° to 28° F		Pave. Temp. 15° to 23° F		Pave. Temp. Less than 15° F	
	Initial	Subsequent	Initial	Subsequent	Initial	Subsequent	Initial	Subsequent
Frost	100	50-100	100-150	50-150	100-200 ²	100-150 ¹	100-300 ^{1,2}	100-200 ^{1,2}
Black Ice	200	100-200	100-300	100-200	100-400 ²	100-300 ¹	200-400 ^{1,2}	100-300 ^{1,2}
Sleet/Freezing Drizzle	200	100-200	100-300	100-200	200-400 ²	100-300 ¹	200-300 ^{1,2}	100-300 ^{1,2}
Freezing Rain	100-300	100-200	200-400	100-200	200-400 ²	200-300 ¹	300-400 ^{1,2}	200-300 ^{1,2}
Dry Snow	100-200	100-200	100-300	100-200	Plow Only ¹	Plow Only ¹	Plow Only ¹	Plow Only ¹
Wet Snow	200	100-200	100-300	100-200	200-400 ²	100-300 ¹	200-400 ^{1,2}	200-400 ^{1,2}

- Mechanical means of snow removal is the preferred method. Before applying any de-icing agents, the surface should be cleared of as much snow and ice as possible by mechanical means.
- Application rates are "MAXIMUM RECOMMENDED RATES". Only apply the amount of pre-wetted salt necessary to accomplish the desired level of service. Rates may vary with regard to pavement temperature, type of roadway surface, and weather conditions.
- Abrasives should not be used on roadways where speeds in the sanded areas exceed 45 mph.
- When wind speed is over 15 mph, use caution when salting and applying moisture drawing de-icing agents.
- ¹ Intersections and low speed hazardous areas may be treated with pre-wetted abrasives when warranted.
- ² If necessary, use alternate de-icing agents like calcium chloride and magnesium chloride in combination with a lower application rate of salt.

11/08

Section B – WisDOT Pollution Prevention Analysis Process

Section B - WisDOT Pollution Prevention Analysis Process

B.1 Introduction

WisDOT must, as one of the requirements in the TS4 Permit, evaluate the pollutant loading from its facilities and determine if those facilities meet the required stormwater pollutant reduction limits as described in Section 2.6 of the TS4 permit. This loading analysis determines the pollutant discharge assuming no controls (the base condition) and with existing controls. The percent reduction between these two levels is compared to the required pollutant reduction to meet and allows WisDOT to focus any additional pollutant reduction efforts, if needed, on the appropriate projects.

WisDOT is continuing a TSS (Total Suspended Solids) analysis on those state highway sections that are not connected highways and that are within municipalities greater than 10,000 people or within EPA defined urbanized areas. The mapping extents for the analysis are based upon the EPA year 2010 urban areas and municipal limits as of 2011. The analysis is based upon the procedures developed to determine municipal pollutant loadings, with modifications intended to focus on the WisDOT highway system needs and characteristics. The Department expects to use the 2020 urban areas from the EPA in the statewide TMDL modeling planned to be completed in FY 2025. The final required mapping limits will be determined in discussions with DNR staff.

B.2 Loading Analysis Process

The loading analysis was split into three parts. Part 1 was completed in 2011 using WinSLAMM version 9.5 for the communities of Superior, Menomonie, and River Falls as well as much of the Northeast and North Central regions. Part 2 was completed in 2013 using WinSLAMM version 10 for the remaining areas. Part 3 was completed in 2020, and included new areas mapped in 2019. WinSLAMM is an urban water quality program that was developed to evaluate the relationships between sources of urban runoff pollutants and stormwater runoff quality. The program can identify pollutant sources and evaluate the effects of different stormwater control practices and runoff management strategies. Control strategies include wet detention ponds, dry detention ponds, infiltration basins, bioretention basins, infiltration trenches, vegetated filter strips, grass swales, porous pavement, street cleaning, catch basin cleaning, and roof and pavement drainage practices. The practices can be applied individually or in combination at source areas, within the drainage system, or at outfalls. The main difference between the two WinSLAMM versions for the WisDOT loading analysis is that version 10 uses more recent highway pollutant loading data (applied using the High Traffic Urban source areas) developed for WisDOT by the Wisconsin Department of Natural Resources (WDNR) and the United States Geological Survey (USGS) than the earlier version.

The 2011 Part 1 analysis in WinSLAMM v 9.5 used standard land use characteristics that describe typical highway sections and the associated parameters that the program needs to develop pollutant loads and evaluate stormwater control practices. These parameters include soil type, slope, paved surface and shoulder areas, and average daily traffic. Each drainage area for each highway section in the MS4 or municipal area was defined using these parameters in a GIS database, as described in the next section. This information was processed to develop a planning level analysis (.pla) file that WinSLAMM used to calculate pollutant loadings.

The data for the 2013/2014 Part 2 analysis in WinSLAMM v 10 was derived from the storm sewer and drainage mapping that has been developed for each WisDOT region. Part 2 also used standard land use data but did not require ADT information. These areas are listed in Table 2.6.1.3. The mapping data for the Part 2 analysis was collected in 2013 and is shown in Appendix E under the 1/10/17 Basin Map Index bookmark.

The data for the 2020 Part 3 WinSLAMM v 10 loading analysis was derived from the storm sewer and drainage mapping done in 2018 and 2019 for each WisDOT region. This data reflected additions to the mapping limits from the 2010 EPA urban areas. These areas are listed in Table 2.6.1.3. The mapping data for the Part 3 analysis is shown in Appendix E under the 3/4/20 Basin Maps bookmark.

B.2.1 Rock River TMDL TSS and TP Analysis

A TMDL loading analysis was performed for the I-39/90 Corridor as described in the Stormwater Quality Management section of Chapter 19 of the I-39/90 Corridor Manual. Rock River TMDL load reduction requirements were determined for each reach shed in the TS4 areas for both total suspended solids and total phosphorus, and for areas outside of the TS4 for total suspended solids. Appendix P includes the load reduction calculation tables, subtopics 1, 2 and 3 of the Water Quality Management section of the Chapter 19 manual, and the corridor maps that show the locations of I-39/90 reach sheds. The results of the analysis and shows that though all reach sheds did not meet the TSS load reduction goal, the corridor as a whole was 17% over the TSS reduction percentage target and that the corridor as a whole was 5% under the total phosphorus reduction percentage target. The TSS and TP reductions from the corridor analysis was incorporated into the SW Region loading analysis described in Table 2.6.1.1 and Table 2.6.1.3 with the other statewide loading updates.

B.3 Mapping and Source Area Data Processing

All regional mapping went through an initial processing phase where the data was checked for relevant information (drainage boundaries, soil type, cross section, slope, and average daily traffic) as well as for topological correctness and coordinate precision.

The sub basin drainage area data was collected using the following process:

1. Each WisDOT owned highway/freeway within a municipal or urban area was divided into numbered drainage areas during the WisDOT statewide stormwater mapping process. Any sub basins less than one acre were combined with the adjacent drainage basins so that each sub basin area is at least one acre.
2. The highway number, sub basin area number, highway segment length, cross section type, slope type and conveyance system type information for each sub basin was entered into the GIS database after both a field and ortho photo review.
3. The Hydrologic Soil Group (HSG), based on NRCS data, and the average daily traffic (ADT) value (from WisDOT references) for each section were entered into the same GIS database for the WinSLAMM v 9.5 analysis. The ADT values are not needed in the WinSLAMM v 10 analysis.

Once the attribute coding was reviewed, the drainage areas were modified to match an equivalent highway segment. The initial revisions to the sub basin data sets involved grouping the polygon areas by their common values, and thereby reducing the number of polygon

drainage areas that would be modeled. After completing the graphical revisions to the GIS files, .pla files were generated based on the summed drainage basin attribute tables. However, after a quality control review, it was determined that this method of analysis did not accurately reflect the percentages of impervious to pervious area for many of the drainage subbasins because the pervious areas from the measured area data extended further than the defined typical cross section widths, and so were not being modeled properly.

To correct this problem, the sub basins were adjusted by dividing each sub basin using the drainage area, soils, slope, cross section, and for the Part 1 analysis, ADT attributes into independent segments mapped to highway length. For example, the original data set may have had three different soil types that were parallel to the highway alignment. Since this data format would result in inaccurate modeling results, equivalent area boundaries representing the different soil types were created perpendicular to the alignment and coded with the appropriate soil type. Thus, the soils area and other characteristics are the same as before, but the defined “boundary” is different. This process allowed the model to accurately account for the pervious area of each sub basin area and was applied to each of the drainage sub basins. Figure 1.4.3.1 illustrates how the adjustment was implemented for the soil types in a drainage basin. This sub basin area adjustment process also included both slope and ADT conditions. The adjusted equivalent sub basin drainage area maps for the completed drainage areas are included in Section E.

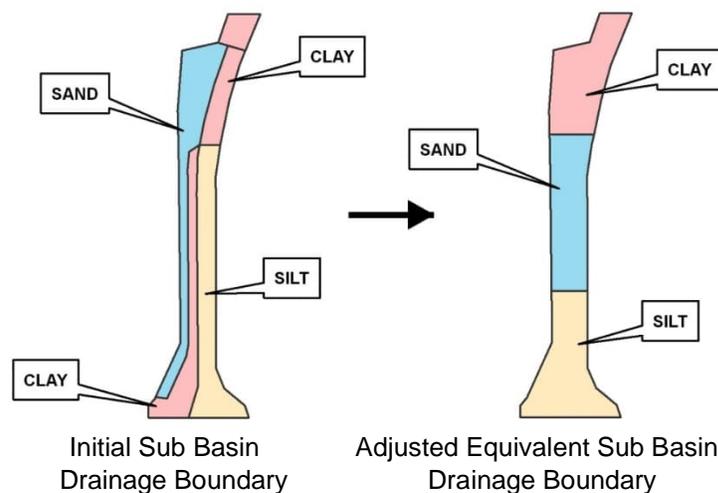


Figure 1.4.3.1: Equivalent Drainage Boundary Adjustment Process

B.4 Modeling Procedure

Once the data describing each urban or municipal area has been collected and processed as described above, the pollutant loading for the base condition and the existing condition are calculated using a two-step process. The first step is to develop the standard highway land uses that describe typical highway sub basin conditions, and the second step is to develop the planning level analysis files to calculate the loading.

The Standard highway Land Use (SLU) files used in WinSLAMM in Part 1 in 2011 for the analysis were developed using the process and criteria described below. These criteria were developed in consultation with WDNR stormwater unit staff. The set of 100-acre SLU files for each relevant combination of cross section type, ADT, slope and soil type was developed using

the WinSLAMM Freeway land use (assuming only paved land and shoulder area and large turf area source areas) and the following assumptions:

- a. Area adjustment
 - i. Freeways – Increase length and area of the paved lane and shoulder area of each rural cross section and decrease turf area by 2% to account for interchanges.
 - ii. Highways – Increase length and area of the paved lane and shoulder area of each urban cross section and decrease turf area by 5% to account for intersections, driveways and sidewalks.
- b. Dynamic Infiltration Rates (for grass swales)
 - i. Clay: 0.04 in/hr (HSG C/D)
 - ii. Silt: 0.18 in/hr (HSG B)
 - iii. Sand: 2.1 in/hr (HSG A)
- c. Slopes (for grass swales)
 - i. Flat: 0%-0.5% - use 0.25% (S1-Freeway Drainage System Type 2)
 - ii. Mild: 0.5%-2% - use 1.25% (S2-Freeway Drainage System Type 2)
 - iii. Moderate: 2%-4% - use 3.5% (S3-Freeway Drainage System Type 3)
 - iv. Steep: >4% - use 4% (S4-Freeway Drainage System Type 4)
- d. Grass swale length – Use the highway segment length times the number of swales in the cross section.
- e. Average Daily Traffic (ADT) for the Part 1 Analysis – Use typical ADT values for each highway segment from the Wisconsin WISDOT2002 Traffic Map, using the following ranges:
 - i. 0-999 vehicles per day – use 500 vpd (ADT0)
 - ii. 1,000-1,999 vehicles per day – use 1,500 vpd (ADT1)
 - iii. 2,000-4,999 vehicles per day – use 3,500 vpd (ADT3)
 - iv. 5,000-9,999 vehicles per day – use 7,500 vpd (ADT7)
 - v. 10,000-19,999 vehicles per day – use 15,000 vpd (ADT15)
 - vi. 20,000-39,999 vehicles per day – use 30,000 vpd (ADT30)
 - vii. 40,000-69,999 vehicles per day – use 55,000 vpd (ADT55)
 - viii. >70,000 vehicles per day – use 70,000 vpd (ADT70)
- f. Grass height – 6.0 inches
- g. Grass retardance factor – C

The same standard land use database was used in the Part 2 and Part 3 analysis with WinSLAMM v 10. However, the highway land use data was applied in a database format and highway areas were modeled as High Traffic Urban source areas rather than Paved Lane and Shoulder source areas. High traffic urban source areas were used to take advantage of the more current highway pollutant runoff data developed by the WDNR and USGS. These high traffic urban source areas do not utilize ADT values as part of the loading calculations.

These criteria were applied to each of the eleven typical cross sections. These cross sections, which are illustrated in Appendix K, represent typical or average cross sections found in the urban areas throughout the state. The initial paved and unpaved surface values used to define these cross sections are described in Appendix K, Standard Highway and Freeway Cross Sections Table.

Part 1 – 2011 Loading Analysis Description using WinSLAMM v 9.5

The loading analysis completed in 2011 was performed by creating .pla files that incorporate the attributes of each sub basin, and then running those files in the WinSLAMM v 9.5 batch processor to evaluate the pollutant loadings for each municipality or urban area.

The .pla files were developed by combining the drainage area and standard land use file information to describe each highway sub basin. The impervious area for each sub basin is calculated by multiplying the highway length with the appropriate highway cross section width. The pervious area then is then calculated by subtracting the impervious area from the highway length times the total highway cross section width. The contents of a typical .pla file are shown in Table 1.4.4 below.

Table 1.4.4 – .pla File Contents

Line Number	Soil Type	Unique Sub Basin ID	Highway Miles in Sub Basin	Total Sub Basin Drainage Area (ac)
01	CLAY	FRE8ADT7S1	0.334	3.970
02	CLAY	FRE1ADT7S1	0.210	2.589
03	CLAY	FRE1ADT7S3	0.346	5.371
04	CLAY	FRE5ADT7S2	1.083	19.394
05	CLAY	FRE5ADT7S3	0.687	10.706
06	CLAY	FRE5ADT7S1	0.333	5.327
07	CLAY	FRE5ADT7S2	0.379	7.673
08	CLAY	FRE1ADT3S2	0.481	13.305
09	CLAY	FRE7ADT3S2	0.447	5.032

The unique sub basin ID describes the cross-section type, ADT value and slope. For example, the unique sub basin ID “FRE8ADT7S1” in the first row defines a segment with cross section Type 8, an ADT classification of 7 (7,500 vpd) and an S1 (flat) slope.

The WinSLAMM batch processor runs the .pla file to analyze the base condition and existing condition loadings. The base case for the regulated state highway system is evaluated by summing the pollutant loading for each sub basin within the highway right-of-way, assuming no grass swales (under the base case all drainage systems are without swales). For those sections with grass swales, the base case is modelled by assuming the system is drained by curb and gutters, valleys, or sealed swales in fair conditions.

Mapped sections that were modeled using this methodology are shown in Appendix E and dated 2011.

Part 2 – 2013/2014 and 2019 Loading Analysis Description using WinSLAMM v 10

The 2013/2014 and 2019 analyses were performed by creating .mdb files in version 10 of WinSLAMM that model all the sub basins draining highway segments in each region. A unique model is created for each numbered highway within each county. Where curb and gutter is not present, grass swales are added to treat sub basins using mapped soil type, longitudinal slope, and cross section. The individual models within each county are modeled separately to obtain the base loading and existing percent reductions for sediment and phosphorus for each numbered highway.

Each sub basin has a unique sub basin ID that describes (in order) the county, numbered road name, sub basin number, cross section type, longitudinal slope, and soil type. For example, the

unique sub basin ID “18_WIS37_08_1S2A” defines a sub basin in County 18 (Eau Claire County), on WIS 37, with a unique sub basin number of 08 within the county, and with a cross section number 1, a slope S2, and a soil type A. An example of the mapping developed for each highway section in each county is illustrated below in Figure 1.4.4.1.

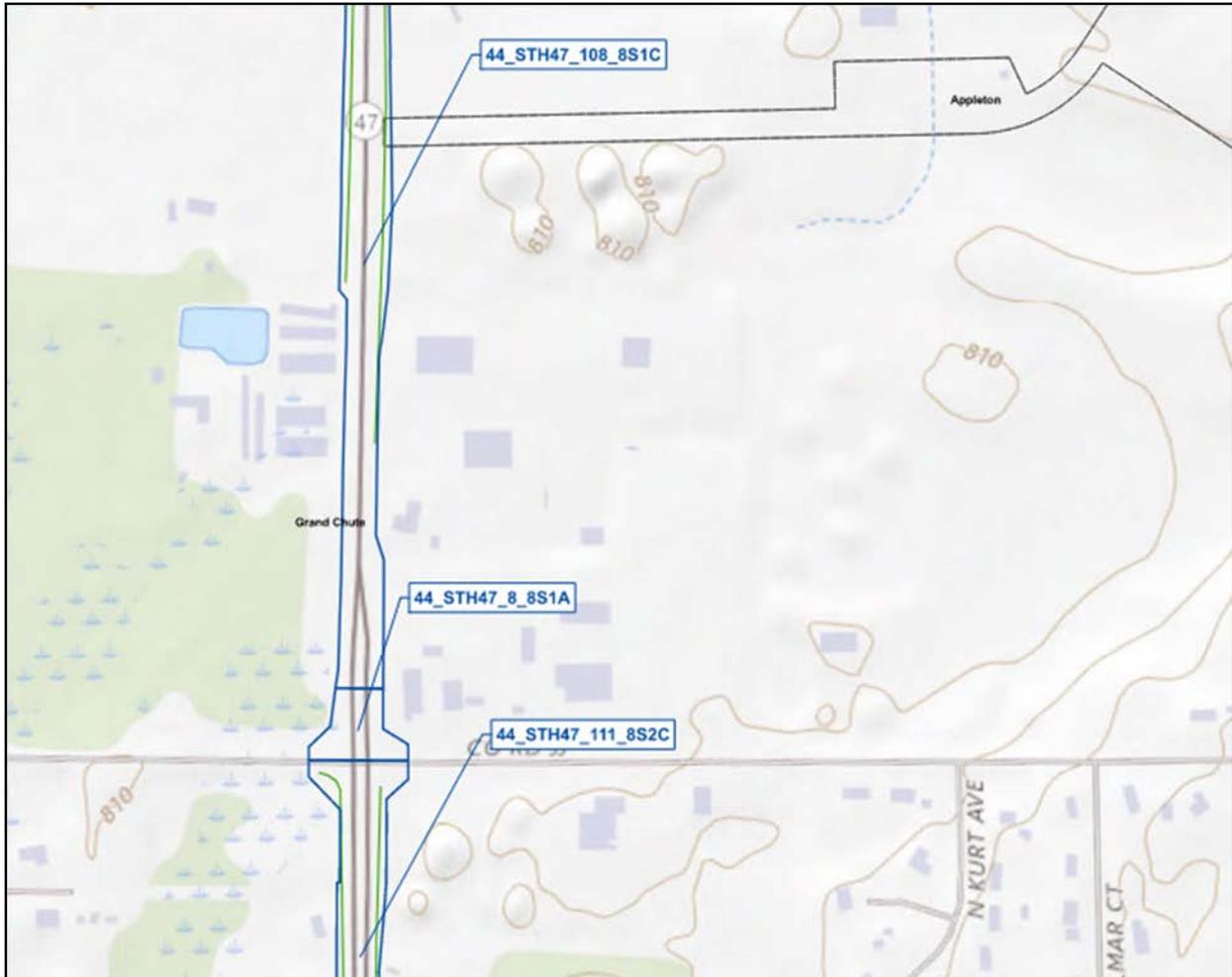


Figure 1.4.4.1: Highway Section Mapping Example

Land Use Label	Standard Land Use Code	Land Use Number Type	Sandy Land Area	Silty Land Area	Clay Land Area	Total Street Length (miles)	Paved Lane and Shoulder Width	Curb-miles per Mile
18_STH312_84_3S3B	HTU Type 3	6	0.0	5.5	0.0	0.019	68	4
18_STH312_89_3S3C	HTU Type 3	6	0.0	0.0	12.4	0.034	68	4
18_STH37_07_1S2A	HTU Type 1	6	27.1	0.0	0.0	0.085	68	4
18_STH37_08_1S2A	HTU Type 1	6	10.8	0.0	0.0	0.046	68	4
18_STH37_13_1S2B	HTU Type 1	6	0.0	3.8	0.0	0.018	68	4
18_STH93_02_1S1A	HTU Type 1	6	17.1	0.0	0.0	0.089	68	4
18_STH93_06_1S2A	HTU Type 1	6	17.1	0.0	0.0	0.070	68	4

Figure 1.4.4.2: Example of Spreadsheet Table Exported from GIS Mapping

The data from this GIS mapping is exported into a spreadsheet that defines, for each sub basin, a label, standard land use code, land use type number, area of each soil type, and street and highway length information. Figure 1.4.4.2 is an example of the spreadsheet table.

This data is then imported into a WinSLAMM model developed for each highway and run to evaluate the pollutant load for each highway within each county for existing conditions both with and without grass swales. Figure 1.4.4.3, below, shows the WinSLAMM program graphical user interface (GUI) that represents the drainage areas and grass swales treatment.

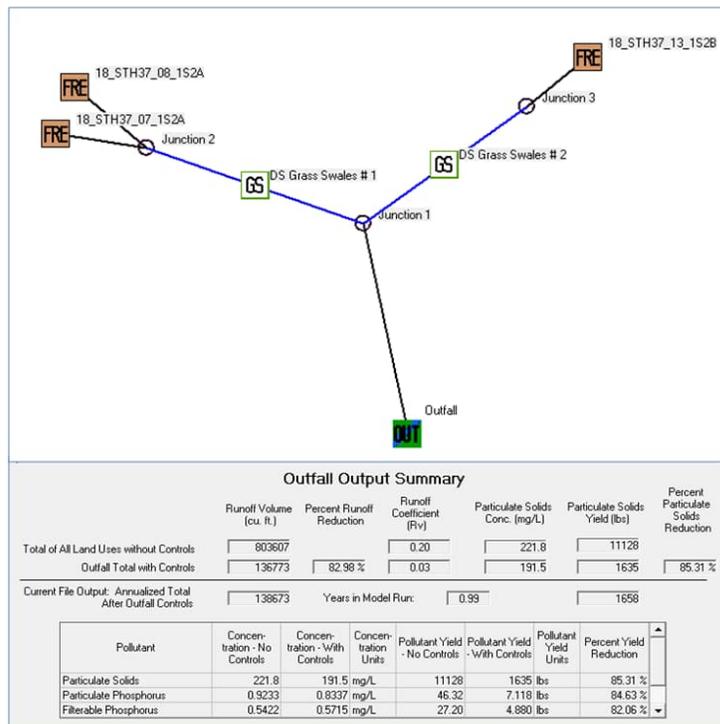


Figure 1.4.4.3: WinSLAMM Graphical User Interface

The existing condition for the highway system is evaluated by summing the pollutant loading for each sub basin within the highway right-of-way. The analysis:

1. Assumes grass swales for all rural drainage systems with less than steep (S4) slopes. S4 slopes are assumed to be ineffective for runoff and pollutant removal.
2. Defines the dynamic infiltration rate for each segment based upon the soil type for that segment.
3. Uses the “grass swale” option if a segment is drained by a curb/gutter/inlet system to a grass swale in the right of way.
4. Calculates the percent TSS loading reduction based upon the difference between the base case and the existing conditions.
5. Models all highways for TSS and Total Phosphorus, even though only TSS will be used in the evaluation.

The grass swale used for this analysis is the typical WisDOT swale, which is a triangular swale with 4:1 side slopes, a class C retardance value, and a typical grass height of six inches. The longitudinal slope and dynamic swale infiltration rate vary depending upon, respectively, the slope condition and the dynamic swale infiltration rate defined for each segment during the mapping process.

The remaining regions modeled in Part 2 and Part 3 are shown in Appendix E.

Part 3 – Incorporating I-39/90 TSS Loading Reductions into SW Region Calculations

The particulate solids yield loading analysis completed for the Rock County and Dane County I-39/90 Corridor in 2021 was integrated into the total modeling results by modifying the previous years' .mdb files in WinSLAMM Version 10.5.077. The extents of the I-39/90 corridor were scanned in ArcMap for those basins used in 2013/14 and 2020 WinSLAMM modelling, and the particulate solids yield loads from the corridor models were removed from their respective WinSLAMM models. The I-39/90 Corridor particulate solids yield results were then added to the Dane County and Rock County values in Table 2.6.1.3 and Appendix I within this report. These tables now include the 2021 I-39 corridor right-of-way TSS data to determine the most recent WisDOT loading reduction statistics. Note that the particulate, filterable and total phosphorus values were not included in the table adjustments because the WinSLAMM model output does not include readily available values for these pollutants in the model output with the modelled control practices.

The Southwest Region particulate solids yield percent reduction increased from 40% to 41% after incorporating the I-39/90 Corridor pollutant analysis into the region pollutant loads. There was no significant change in the statewide reduction results. The I-39/90 Corridor Manual section (Chapter 19) on Drainage, Erosion Control and Stormwater Quality is included below to illustrate how the TSS reductions for the Rock River TMDL, were applied.



CHAPTER 19: DRAINAGE, EROSION CONTROL, AND STORMWATER QUALITY

Chapter Contact: John Voorhees, Drainage Engineer
Issued: May 16, 2012
Revised: September 1, 2015; February 9, 2015; August 28, 2014; July 30, 2014, May 8, 2014, February 25, 2014, August 14, 2013, May 30, 2013; April 4, 2013; November 27, 2012; June 19, 2012

SCOPE/PURPOSE

The purpose of this chapter is to document the design criteria established for I-39/90 corridor that should be used by all I-39/90 design staff involved in hydraulic analysis of bridges, culverts or storm sewers along the mainline, side roads, and at interchanges. These criteria were developed from the FDM and WisDOT Region and Central Office staff. In cases where it's not clear which criteria to apply, please consult John Voorhees of AECOM. It is important to be as consistent as possible throughout the corridor to provide a quality design and simplify construction.

The proposed drainage system must also be evaluated for its ability to maintain drainage during construction. Because the construction staging for each project within the corridor will differ, there will not be a specific set of criteria to address construction drainage issues. Any construction details developed by individual project teams for drainage during construction or for the final drainage system should be available for corridor-wide use. Send construction details to John Voorhees at AECOM, who will add them to this manual and the SharePoint library.

All drainage design needs to be coordinated through the I-39/90 CMT, local Drainage Districts, and the WDNR liaison. The drainage design also needs to meet the requirements in the DOT/DNR Cooperative Agreement contained in FDM Section 20-30-1, Federal and State Environmental Laws, Policies, Regulations and Agreements, the USACE, FEMA studies, the Rock River TMDL study, the project environmental document and any other relevant local studies.

REFERENCE DOCUMENTS

- FDM Section 10
- FDM Section 13

ATTACHMENTS

Attachment Number	Description
19-3	WisDOT TMDL Stormwater Report Form v5.xlsm
19-8	Relevant sections of the Draft Wisconsin Interstate Highway Flood Potential Survey
19-9	Rainfall Data for Use with the I-39/90 Corridor Project
19-10	Compost Filter Berm SPV
19-11	Upslope Tracking SPV



- 7) Include an SPV for tracking upslope to reduce erosion and to clarify when permanent erosion control measures need to be placed with each corridor plan. The SPV is in [Attachment 19-11](#).
- 8) When specifying the fertilizer for corridor projects, use Type A fertilizer.

Stormwater Quality Management

- 1) The corridor project must meet TSS (Total Suspended Solids) reduction requirements defined in TRANS 401 as well as the requirements defined in the Rock River basin TMDL report. The TMDL report sets reduction requirements for the entire Rock River basin, which includes the I-39/90 corridor project area, for TSS and Total Phosphorus (TP). For the purpose of the water quality analysis for this project, the I-39/90 highway corridor, including the alternate routes, is divided into three sections – those DOT highway sections that pass through MS4 (Municipal Separate Storm Sewer System) areas, those that do not, and town and county roads. TMDL requirements only apply to those portions of the corridor that are within the MS4 areas. TRANS 401 requirements apply to the entire corridor. The TMDL requirements are generally (but not always) more restrictive than TRANS 401 for post-construction TSS and TP control. Attachment 19-18 is a set of 11 maps (a corridor overview map and 10 detailed maps) that illustrate where the TMDL compliance limits are. The table below indicates the percent reduction of TSS and Total Phosphorus (from a “no management” condition) that are required for each reach shed along the corridor under the TMDL program.

MS4 Municipality	Reach Shed Number	TMDL Section of Corridor Length in Reach Shed (mi)	TRANS 401 Section of Corridor Length in Reach Shed (mi)	Percent Total Phosphorus (TP) Load Reduction	Percent TSS Load Reduction	Notes
Madison	66	1.50	4.34	70.4 %	72.2 %	May be combined with non-MS4 area in reach shed
Janesville	61	0.86	13.71	N/A	40.0 %	No added TMDL reductions required beyond TRANS 401
Janesville	71	0.35	0.00	57.6 %	64.3 %	No non-MS4 area in reach shed
Janesville	73	4.70	0.89	80.9 %	77.5 %	May be combined with non-MS4 area in reach shed



Wisconsin Department of Transportation

111 Interstate Blvd., Edgerton, WI 53534-9399 (608) 884-1234 FAX (608) 884-1220 www.dot.wisconsin.gov



MS4 Municipality	Reach Shed Number	TMDL Section of Corridor Length in Reach Shed (mi)	TRANS 401 Section of Corridor Length in Reach Shed (mi)	Percent Total Phosphorus (TP) Load Reduction	Percent TSS Load Reduction	Notes
Janesville/Beloit	76 & 79	1.13 (CTH G)	N/A	85.3 %	67.9 %	For STH 11 and CTH G Segments in Janesville MS4 Area only
Beloit	81	3.23	2.05	39.3 %	40.0 %	May be combined with non-MS4 area in reach shed

The corridor sections in reach sheds 66, 73, 79 and 81 are split between TMDL and TRANS 401 analysis areas. If it is not practicable to meet the total load reductions in the TMDL areas, designers may increase the load reductions in the TRANS 401 (non-MS4) areas of the same reach shed and apply any reductions over the 40% TSS load to the TMDL areas. If the required reductions are still not practically feasible, then contact John Voorhees who will review them with the DNR to determine how to proceed.

- 2) The highway areas that do not pass through MS4 systems must meet the reconstruction requirements of TRANS 401, which are for a 40% reduction in TSS when compared to no stormwater controls (there are no TP reductions specified in TRANS 401). Those non-MS4 sections that cannot meet the 40% TSS reduction requirement must reduce the TSS load to the maximum extent practicable. Follow the requirements of Trans 401 and DOT/DNR Cooperative Agreement for non-TMDL areas outside of the limits illustrated in the drawings in Attachment 19-18. If the 40% TSS reduction requirement has been met, then the total phosphorus reduction requirement is assumed to be met.
- 3) Certain corridor projects are not required to perform a TMDL loading analysis. Though these road projects do not require a TMDL analysis, WisDOT designers should still strive to incorporate reasonable and feasible stormwater quality treatment practices in their project designs. These projects fall under one of two areas.
 - a. Runoff pollutant load coming from local (county, city, village, town, etc.) road crossings of state-owned highways should be the responsibility of the local authority and not WisDOT. This includes the bridge structures and approaches of those crossings that are within WisDOT ROW.
 - b. WisDOT does not need to evaluate the load or load reduction from county highway or town road projects, or other non-WisDOT-owned road projects, that are a part of the corridor project alternative route designation. WisDOT is not responsible for the pollutant load of these road sections with respect to TMDL waste load allocation requirements. The pollutant load associated with these projects, including those sections used as an alternative route, should remain the responsibility of the county or other governmental



Wisconsin Department of Transportation

111 Interstate Blvd., Edgerton, WI 53534-9399 (608) 884-1234 FAX (608) 884-1220 www.dot.wisconsin.gov



body with respect to TMDL wasteload allocation requirements and MS4 developed urban areas standards.

- 4) Use the new sections in FDM 10-35 (Stormwater Quality Control Practices) when preparing the water quality analysis of the project. The new sections are:
 - 10-35-1 – Stormwater Control Measure (SCM) Selection
 - 10-35-2 – The Effects of Urbanization on Water Quality
 - 10-35-10 – Filter Strips (Draft, Attachment 19-21)
 - 10-35-3 – Project Stormwater Quality Analysis Process (new section)
 - 10-35-15 – Wet Detention Pond Stormwater Quality Design
 - 10-35-20 – Catchbasin Design and Cleaning
- 5) Use the new FDM Section 13-1-10 (Documentation of Hydrologic/Hydraulic Design) for instructions to record the stormwater quality analysis performed for the project. The FDM section, however, does not include the requirement to enter TMDL pollutant loading data into the Stormwater Report water quality forms. [Attachment 19-3](#) is a revised Stormwater Report form that includes a section for entering TMDL pollutant loading data for each of the control practices. To complete the TMDL pollutant loading summary, for each control practice used for a project, enter the calculated TSS and TP loads, with and without controls. The loads should be calculated using an approved model. The spreadsheet will calculate the pounds of TSS and TP removed and transfer the totals to the Stormwater Report WQ-Summary worksheet. Contact the Region 1 Stormwater Engineer or the Corridor Management Team Drainage Engineer for assistance.
- 6) Roadside ditches are a stormwater control measure (SCM) that will be used to achieve stormwater quality requirements for the corridor. Trans 401.106(10)(a)(2) requires a minimum ditch length of 200 feet and design velocity less than or equal to 1.5 ft/sec to provide 80% TSS removal or maximum extent practicable based on a 2-year, 24-hour design storm or on a 2-year design storm with a duration equal to the time of concentration. Storm sewers should discharge to grass swales as frequently as possible for the swales to meet the Trans 401 requirements. Storm sewers must outfall to wet detention basins, biofilter areas or grass swales for TSS TMDL or TRANS 401 reduction credit.
- 7) Per environmental commitments in the I-39/90 Corridor EA/FONSI, attempt to maintain or lower the stormwater runoff discharge rates as compared to predevelopment site conditions along the corridor for the 2-year, 24-hour design storm to the maximum extent practicable.
- 8) Do not construct any open water stormwater quality control practices or other open water drainage practices at the Exit 175 interchange and the Exit 183 interchange. Please review any other stormwater control projects that occur elsewhere within five miles of the Southern Wisconsin Regional (Janesville) Airport with John Voorhees. Any biofilters constructed in these areas must drain within 48 hours. Do not construct any open water stormwater control practices within 5000 feet of the Jana Airport (serving the Edgerton area) or the Beloit Airport. Practices outside of the 5000 foot radius but within the 5 mile radius for either of these airports may be acceptable – contact Chris Egger (christopher.egger@dot.wi.gov) and copy John Voorhees and Jenny Grimes to verify that open water practices are acceptable in those areas. Attachment 19-15 is a map illustrating the five mile limit for all three areas and the 5000 foot limit for the Jana and Beloit Airports.



- 9) Designers should try to use the following TSS and Total Phosphorus reduction treatment options, in this order, when determining the percent load reductions for TSS and Total Phosphorus load reductions for TMDL areas.
 - a. Filter Strips
 - b. Grass Swales
 - c. Filter Strips to Grass Swales
 - d. Rain Garden/Biofiltration
 - e. Wet Detention Ponds
 - f. Catchbasin Cleaning
 - g. Street Cleaning
- 10) Process for Final Project Concurrence for the Water Quality Certification Submittal to the DNR.

The Water Quality Stormwater Report submittal is part of the overall submittal to the DNR for final project concurrence and water quality certification.

CMT Review: The overall submittal for each project should be sent to Jennifer Grimes (I-39/90 CMT Environmental Coordinator) and copied to John Voorhees (I-39/90 Drainage Engineer). Jennifer will forward the complete submittal to the DNR. It should be completed as part of the pre-90% design submittal. When sending the submittal for CMT review, include the link to the SharePoint folder containing the overall submittal and the 90% plan set.

This overall 401 Water Quality Certification submittal includes three parts:

- A. DNR/DOT Project Review Form describing the natural resource impacts and mitigation measures for the project. Examples include the amount and type of wetland impacts, and discussion of impacts/modifications to waterways. Check with the environmental coordinator for other types of impacts.
- B. The Stormwater Report Water Quality (WQ) worksheets, developed as described in this Water Quality section of the Corridor Chapter 19 Drainage Manual.
- C. An erosion control plan design that meets TRANS 401 requirements.

Part A:

The overall submittal for final project concurrence for water quality certification should begin with the DNR/DOT Project Review Form DTNR00002, [Attachment 19-20](#), as the cover sheet for the overall submittal.

On the DNR/DOT Project Review Form, under the category "Type of Review Requested," check "Final Concurrence." Under the category "Stormwater Management," check "Trans 401 post construction requirements," check "NPDES MS4/Urbanized Area" (if applicable), and check "TMDL Implementation Area."

The form should be used to describe how any issues brought up by DNR throughout the project have been addressed.

- If there are no wetland impacts, state that there are no impacts.
- Describe any waterway crossings that have been coordinated with DNR.
- Include discussions of any erosion control measures unique to the project.



Wisconsin Department of Transportation

111 Interstate Blvd., Edgerton, WI 53534-9399 (608) 884-1234 FAX (608) 884-1220 www.dot.wisconsin.gov



- If there are any issues that require special provisions (i.e. adherence to in-stream disturbance or migratory bird protection dates) or notes in the plan, identify them and state how they are addressed in the plan.

Part A Attachments:

- Project Location Map
- Include the initial DNR letter for the project segment, and any other relevant correspondence to clarify project issues, water quality, AOP, stormwater, wetlands, etc.

Part B:

- I. The Water Quality Stormwater Report Submittal contents should begin with the Water Quality Stormwater Report Submittal Cover Letter, [Attachment 19-23](#).

Describe the following on the cover letter and keep the letter to one page (if possible):

1. Where is the project in the corridor?
2. What construction activities are proposed?
3. What are the water quality goals for this project? (Note in the cover letter that the goals were determined from the Corridor TMDL Analysis spreadsheet submitted to the DNR.)
4. What reach shed(s) is the project in?
5. In general, what was done to meet the goal and how well was the goal was met? If the goal was not met, explain why it was not met. List the number of each kind of stormwater control practice included in the submittal.

- II. Stormwater Report WQ-Summary worksheet.

County Road and Town Road projects and County/Town Road crossings of the Interstate require only items I. and II. listed above for Part B. No water quality analysis is required for these types of roadways. Note this in the Stormwater Report WQ-Summary worksheet (item II above).

All Interstate and State Road right of way stormwater measures should include the following additional information III. – VI.:

- III. Stormwater Report WQ – Control Practice Detail Worksheets that have practices used in the project.
- IV. Drainage system sub basin outfall map. The outfall label names should be consistent between the map, any tables, and the model (if one was used).
- V. Drainage summary spreadsheet of model results if a model was used, summarizing the TSS and TP pollutant reduction masses and percentages for each outfall.
- VI. If a model was used, model input files and model output summary files.

Part C:

Include a copy of the Erosion Control Plan (set the .pdf file to print to 11x17 for the EC Plan sheets). At a minimum, this should include the erosion control plan sheets from the plan set. If available, also include any special provisions and quantities.

Section C – Research Project Summaries

Evaluation of Bioretention Swales at Reducing Pollutant Concentrations and Loads in Highway Runoff, Waukesha, WI

Cooperator: Wisconsin Department of Transportation and Wisconsin Department of Natural Resources

Project Chief: Judy A. Horwathich

Location: Statewide

Period of Project: October 2013– 2019

Total WisDOT Funding: \$222,000

Problem

In Wisconsin, State and Federal regulations apply to the quality of stormwater runoff from the State highway system. The Wisconsin Department of Transportation (WisDOT) finalized a Memorandum of Understanding (MOU) in 1994 with the Wisconsin Department of Natural Resources (WDNR) for the control of stormwater runoff flows from the highway system. The MOU covers State-owned and -operated systems in Milwaukee and Madison and includes a phased approach to examine stormwater control opportunities at many other municipal areas. WisDOT also must follow the rules described in TRANS 401, which was most recently updated in February of 2013. In addition, the U.S. Environmental Protection Agency Phase II stormwater regulations focus on the quality of water flow from storm sewers.

TRANS 401.106 requires at least an 80-percent reduction in total suspended solids (TSS) for transportation facilities first constructed on or after January 2003 and a maximum extent possible for reconstructive highway projects. The cost of land in high-density urban areas can be prohibitive for implementation of traditional stormwater systems such as wet detention basins and bioretention systems. This study will evaluate the effectiveness and practical applications for three bioretention designed to improve the quality of stormwater runoff.

This study builds on a long history of U.S. Geological Survey (USGS) urban water-quality investigations in Wisconsin. In 1978, the U.S. Environmental Protection Agency (USEPA) established the Nationwide Urban Runoff Program (NURP) to assess the water-quality characteristics of urban runoff (Bannerman and others, 1983). When the City of Milwaukee, Wis., was chosen by the USEPA as a NURP site, a partnership between the WDNR and the USGS was developed to evaluate urban runoff in Milwaukee. Since the NURP study, the USGS and the WDNR have continued their partnership and have completed more than 15 studies in at least 6 cities to assist the State of Wisconsin in characterization of urban stormwater runoff. Results from this study provide additional information to meet the partnership goals of understanding urban runoff.

Objective

The objective of the proposed study is to determine what type of engineered soil is best at reducing phosphorus export in a bioretention system. The flux of stormwater pollutants will be determined for three different engineered soils. The specific objectives are the following:

1. To determine the concentration and load of typical stormwater pollutants at the inlet and outlet of three bioretention systems.
2. To develop a relationship between engineered soil types and pollutant reduction.
3. To determine how much of the runoff is treated by the systems versus how much is bypassed.
4. To evaluate the effect of particle size on the flux of pollutants in each soil type.
5. To verify the infiltration rate of the engineered soils.
6. To determine if winter road salting has any impact on bioretention.
7. To calibrate and verify the engineered soil treatment calculations in WinSLAMM.

Approach

This study will use a paired basin approach, meaning that data will be collected from three cells for direct comparison. Three bioretention cells will be monitored on Highway VV in Waukesha, Wisconsin. The quality and quantity of the runoff is expected to be similar for these sites, though the linear length might differ. The design of one of the systems will follow the criteria in Wisconsin's technical standard 1004; the other two systems will test layering of engineered soils. All the systems will be planted with the same types of plants.

To isolate the effect of the engineered soils, the bio-retention systems will be lined with heavy plastic. This will eliminate any water from being lost by infiltration. The water will have to leave the device thru the drain tile, overflow, or by evapotranspiration. Soil moisture measurements within each cell will help quantify the importance of evapotranspiration in the operation of each system.

Water quality samples will be collected at the inlet and outlet for each system. Automatic water quality samplers will be programmed to collect flow composite samples. Samples will be collected for a total of 25 events at each site. Events less than 0.2 inches in rainfall depth will probably not be included in the water quality analysis. The constituents list could include TSS, SSC, VSS, PSD, Dissolved Solids, Total P, Dissolved P, Total recoverable zinc, dissolved zinc, total recoverable copper, and PAHs.

Flow will be measured with HS and trapezoidal flumes fitted with pressure transducers to measure the depth of the flow in the flume. Any bypass volume will be determined with a stage recorder installed in the surface storage area above the engineered soil. A rain gage will be located on

site to measure rainfall depth and intensities. Campbell CR1000 data loggers will be programmed to control the sampling equipment and store the data. Data will be retrieved twice per day and migrated to the USGS real-time data website. Double ring infiltration testing will be conducted throughout the study.



Figure 4. Bioretention sites in Highway VV median strip

Progress (Jan 2014 – May 2016)

In spring of 2014, flow monitoring and sampling equipment was installed to measure highway flow (inlet) and bioretention tile flow (outlet) at each of the two cells. Monitoring equipment has been functional for the last seven months. The expectation was that most of the water measured through inlets would be observed at the outlets. However, after several inlet flow events, minimal outlet flow events were recorded at the bioretention outlet. The only water-quality sample collected at the one outlet occurred during a 4-in. rainfall. The reason for lack of flow through the outlet appears to be the ability for the cells to function effectively even with the very low infiltration rate of the native soils. By running WinSLAMM for a range of infiltration rates and rainfall depths, it was discovered that an infiltration rate as low as 0.5 in/hr. was sufficient to eliminate discharge from the drain tiles. One reason the infiltration rate can be so low is because the ratio of watershed size to cell size is roughly 5 to 1.

Plans (June 2016 – December 2017)

Monitoring of the site will end in June 2016. A report summarizing the performance of the bioretention system will be written. Results and recommendations will be presented to the WisDOT staff for review.

Note: Because of Waukesha Bioswale performance issues, funding dollars were shifted to:

Verification of Grass Swales Performance – Phase II

Background

State departments' of transportation (DOTs) are implementing vegetative practices to meet federal and state requirements for flow, sediment, and other pollutants. To determine the effectiveness of vegetative filter strips (VFS) and vegetative grass swales (VGS) at reducing stormwater runoff from urban highways, WisDOT entered into an agreement with the USGS and WDNR. In 2011-15, Phase-I included six sites to determine effectiveness of these vegetative systems along U.S. Highway 51 Madison, WI (South Stoughton Service Road between Pflaum Rd and Buckeye Rd). Three VFS sites captured Highway 51 sheet flow at 5, 10, and 15 feet along the vegetative side slope. A fourth site captured roadway runoff from a curb cut that entered into the VGS. A fifth VGS site measured highway sheet flow and point-source flow from previous sites. And the sixth site represents 500 feet of isolated VGS channel flow all other sites. The extensive dataset includes flow and concentration data for more than 200 and 25 events, respectively.

Phase-I demonstrated the importance of evaluating larger events. Through statistical analysis of the current data, rainfall depth is the primary factor in determining runoff from VFS and VGS. Initial runoff from VFS and VGS usually begins after 1 inch of rainfall. Several rainfalls greater than 2 inches conveyed runoff causing backwater conditions at the lowest VFS and the 500 ft VGS sites. To quantifying these extreme events, a Phase-II study modified sampling sites to capture larger rainfall events. It is critical for DOTs that use swales as conveyance and treatment systems to determine their hydraulic and water-quality effectiveness.

Approach

In 2016, Phase-II will complement the shortcomings of the Phase-I study. Goals include capturing extreme events as well as determining particle deposition along the VGS channel. A recent USGS journal article showed that using inaccurate particle-size distributions can result in over/under estimating removal efficiency of stormwater control practices. NCHRP Report 521 states there is a need to characterize particle size and associated constituents with different-sized particles from highway runoff including heavy metal, nutrients, and hydrocarbons to improve predictive modeling. Phase-II will track flows and particles at five locations along the grass swale study site.

In 2016, the USGS will monitor eight points at the Hwy 51 location. To measure filter strip runoff there are four sites: three lateral filter-strip collectors from Phase-I and a new flume site that was placed in the grass swale channel to measure 600 feet of filter strip. Sample collection will only occur at the flume site. To measure grass swale runoff there are four sites: the roadway point source and typical grass swale sites from Phase-I, and two new sites where the downstream flume from Phase-I is now located 250 feet from roadway site, and a movable sampling point to pinpoint deposition of larger particles after the point source enters into the channel.

Discrete flow-composite sampling will be done at the five locations during measurable runoff events. To reduce cost, the Wisconsin USGS staff will use an in-house particle-size analyzer to analyze particle-size fractions for each discrete sample. In 2016, event mean concentrations of total suspended solids and total, and dissolved phosphorous will be determined for eight runoff events. In 2017, due to WDNR laboratory budget cuts, only five events will be measured.

Progress (Jan 2015 – May 2016)

Between the fall of 2015 and spring of 2016, equipment was installed to start Phase-II. Samples collection began in April 2016. However, it was evident several silted samples had to be discarded due to the bare soil around newly established equipment. Two events were processed using a discrete sampling protocol.

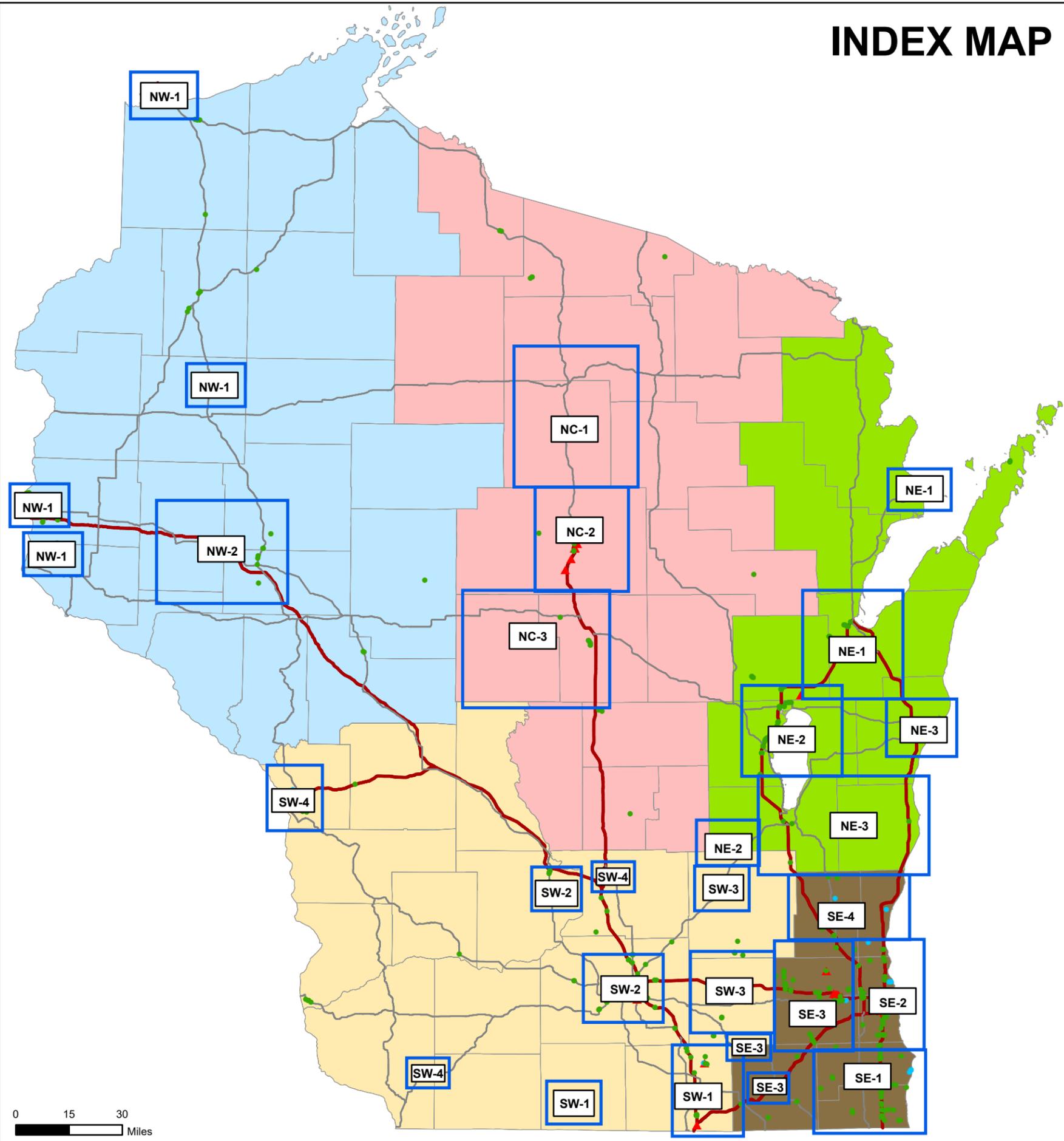
Plans (June 2016 – December 2017)

Monitoring is scheduled to end in June 2017. The USGS is currently in the process of writing a report for WisDOT. The report will include project scope, study design, site description, and effectiveness of filter strips and grass swales at reducing runoff, TSS, and other constituents, as well as details about the WinSLAMM calibration process.

As of 2024 the report data has been placed in the USGS stormwater database.

Section D – Statewide High-Risk Outfall and Stormwater Control Practice Locations

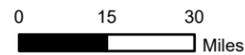
INDEX MAP



Legend

- SCP - Connecting Hwy
 - SCP - Local Program
 - SCP - State
 - ▲ High Risk Outfalls
 - Interstate
 - US Highway
 - County Boundary
- WisDOT Regional Location**
- Southwest
 - Southeast
 - Northeast
 - Northcentral
 - Northwest

- NC-1 Map Sheet ID
- Map Sheet Limits



08/25/2025

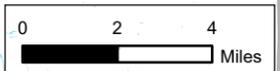
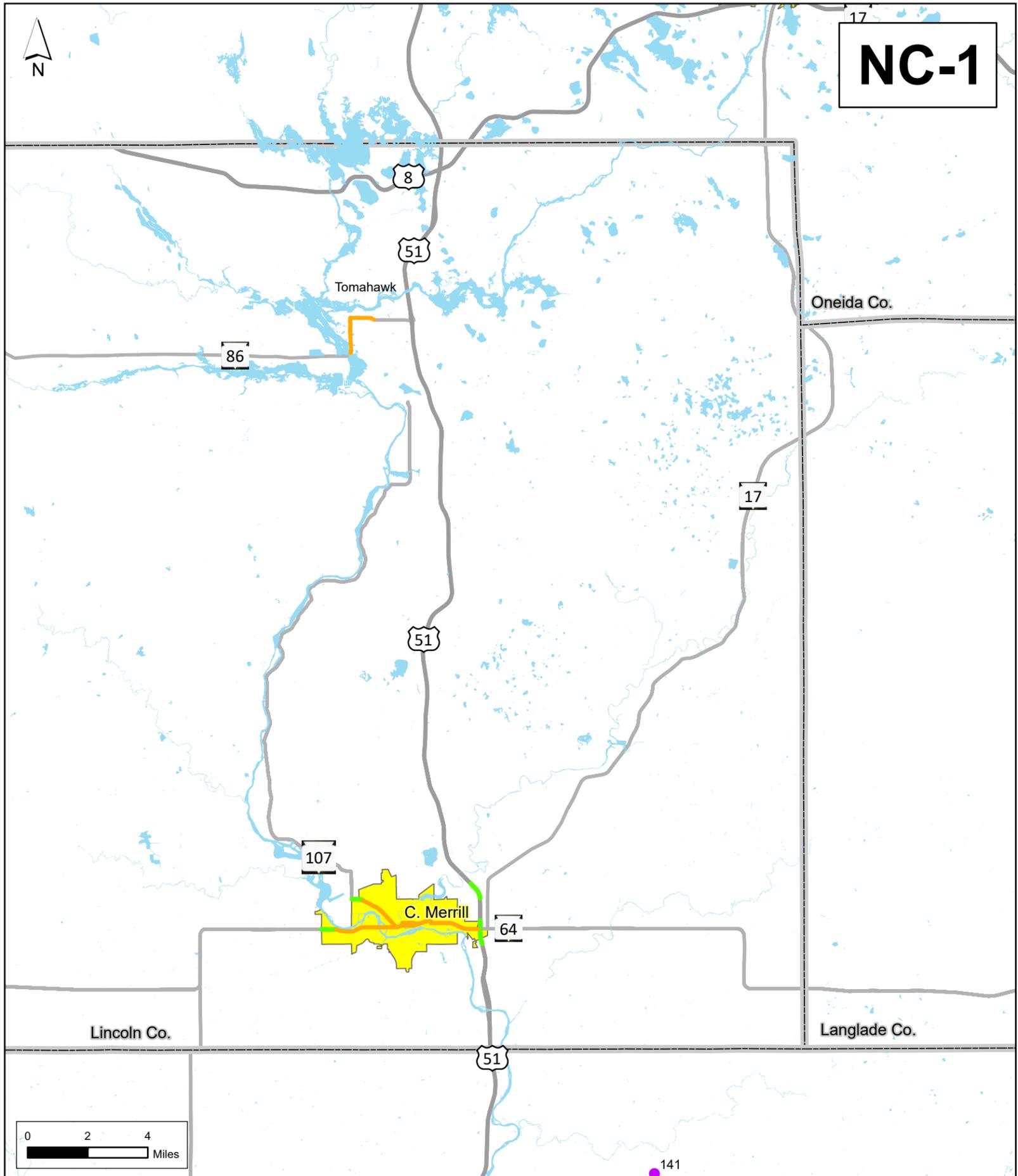
Statewide TS4 Mapping - 2024



WisDOT Regional Index Map
Sheet Locations



NC-1



- ▲ High Risk Outfall
- SCP - Connecting Hwy*
- SCP - Local Program*
- SCP - State*
- Highway
- Connecting Highway (WisDOT 2024)
- TS4 Highway
- Urban Area (2020) and Municipalities with Population > 10k
- County Boundary
- Water

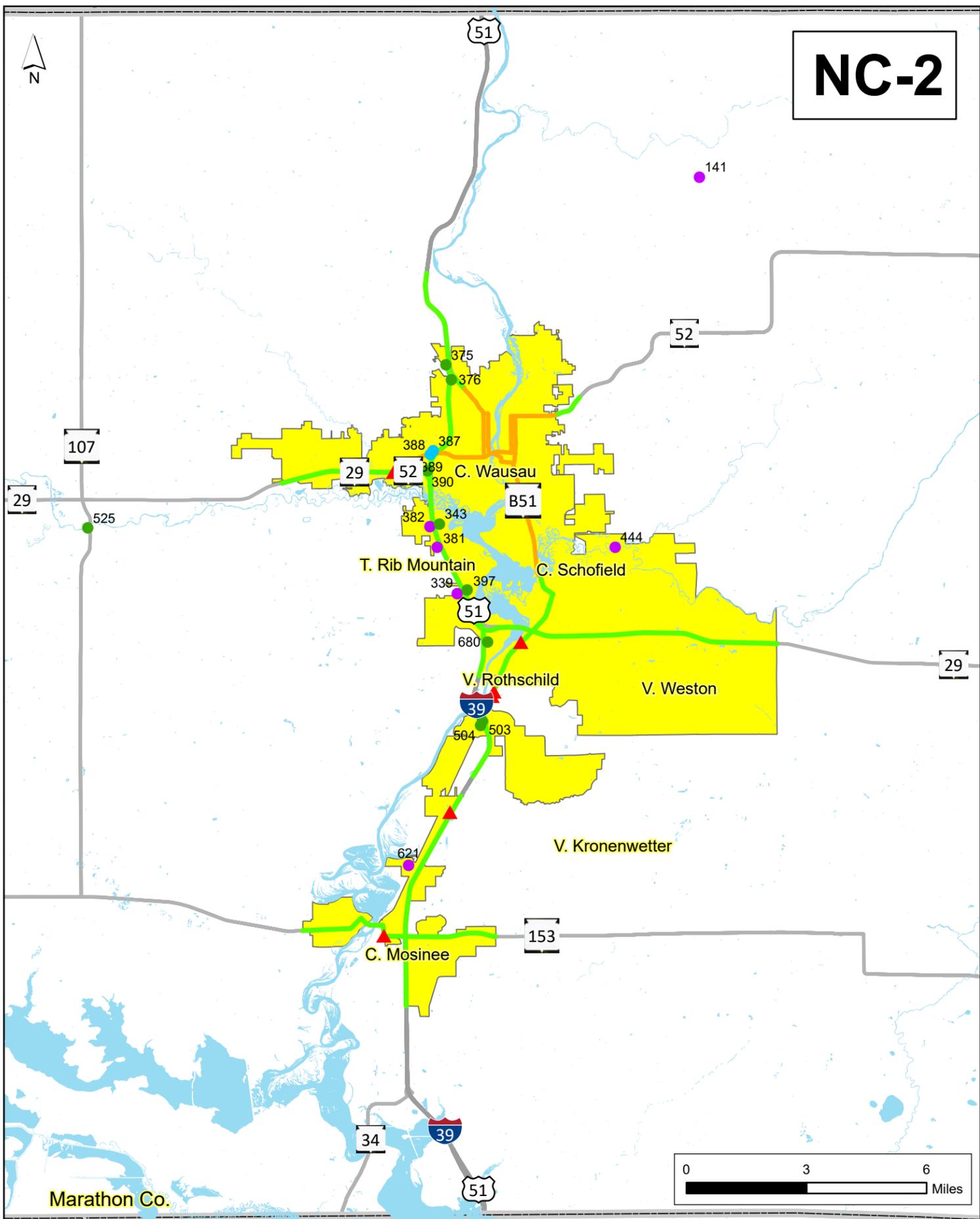
* Label indicates SCP_ID from the SCP Database
Permittee Name (C=City, T=Town, V=Village)

08/25/2025

Statewide TS4 Mapping - 2024

WisDOT North Central Region





- ▲ High Risk Outfall
- SCP - Connecting Hwy*
- SCP - Local Program*
- SCP - State*
- Highway
- Connecting Highway (WisDOT 2024)
- TS4 Highway
- Urban Area (2020) and Municipalities with Population > 10k
- County Boundary
- Water

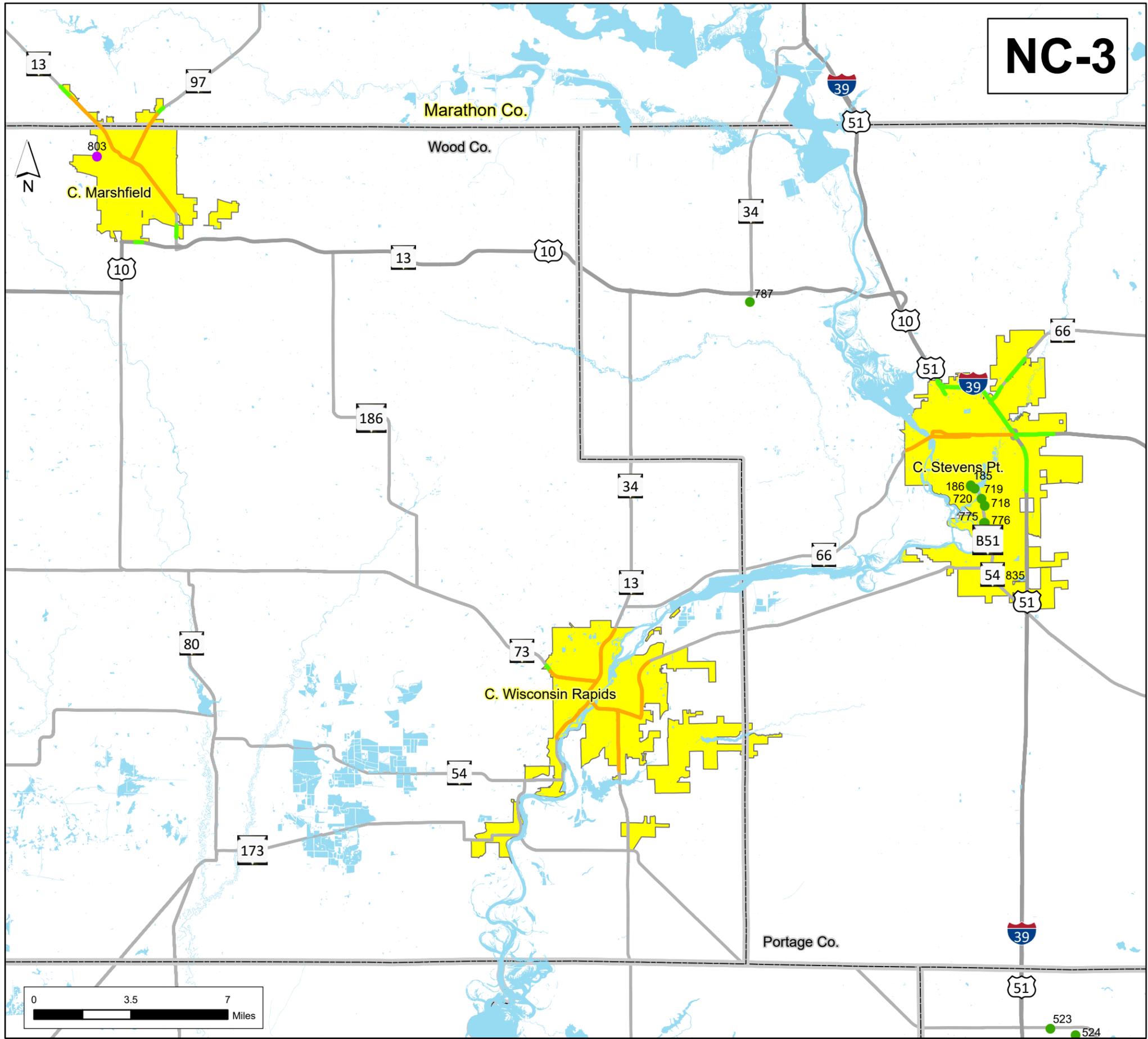
* Label indicates SCP_ID from the SCP Database
Permittee Name (C=City, T=Town, V=Village)

08/25/2025

Statewide TS4 Mapping - 2024

WisDOT North Central Region





NC-3

- ▲ High Risk Outfall
- SCP - Connecting Hwy*
- SCP - Local Program*
- SCP - State*
- Highway
- Connecting Highway (WisDOT 2024)
- TS4 Highway
- Urban Area (2020) and Municipalities with Population > 10k
- County Boundary
- Water

* Label indicates SCP_ID from the SCP Database
Permittee Name (C=City, T=Town, V=Village)

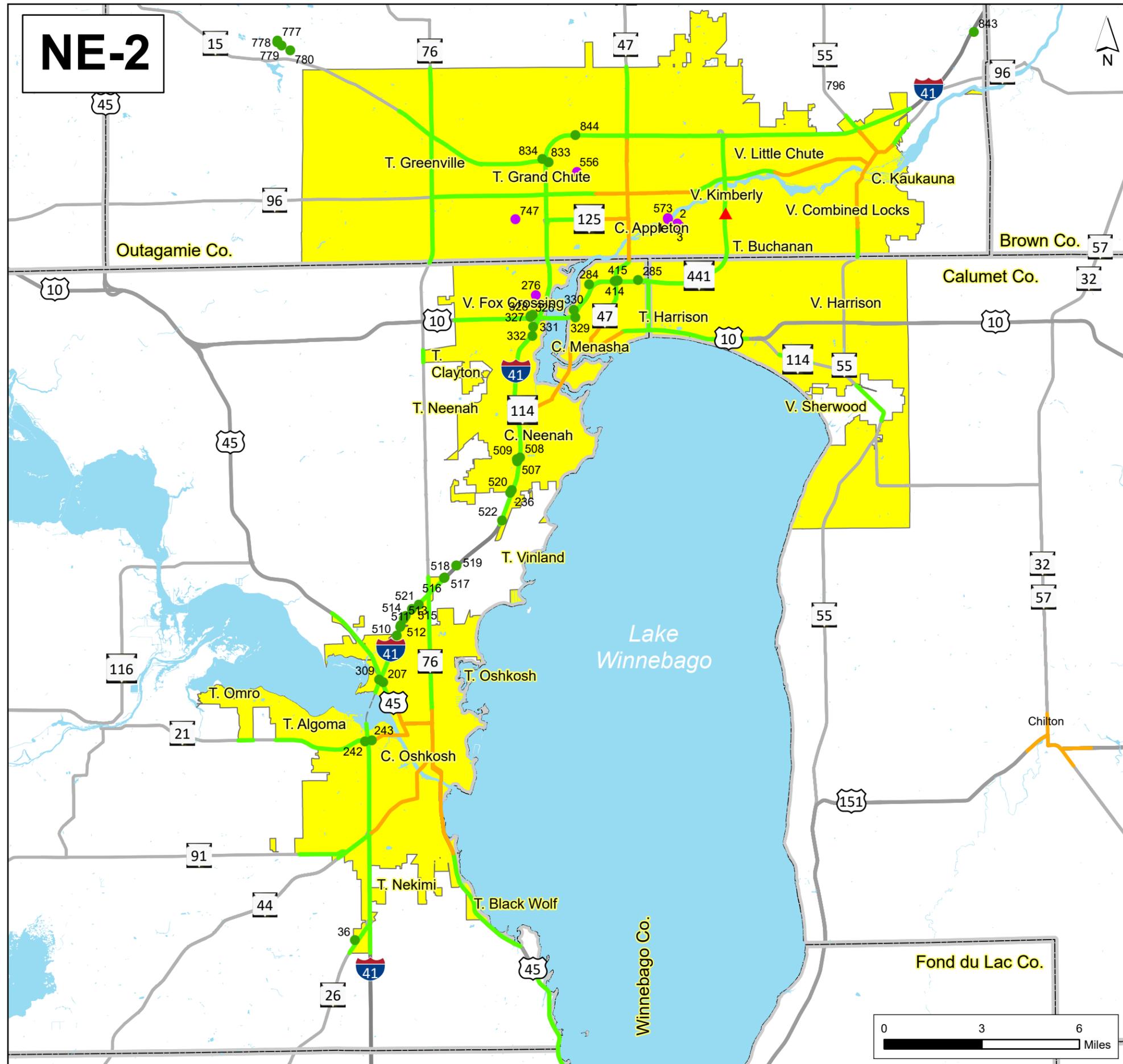
08/25/2025

Statewide TS4 Mapping - 2024

WisDOT North Central Region

Sheet NC-3

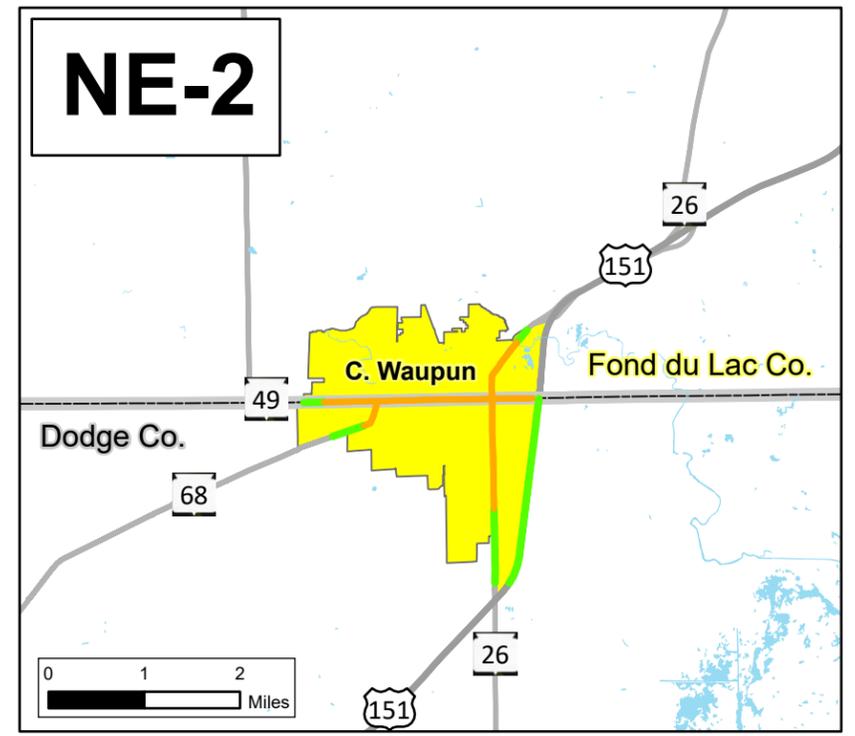
NE-2

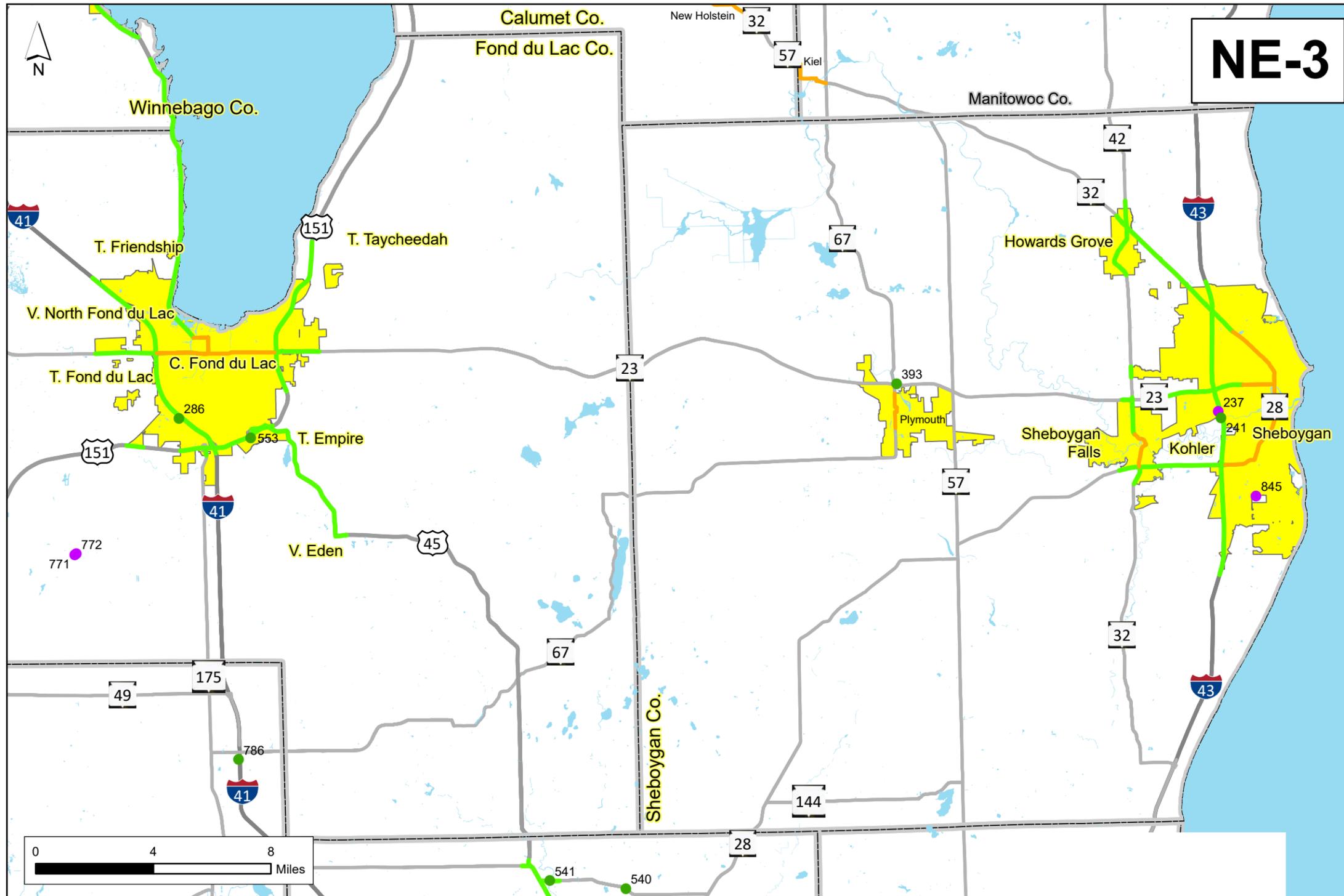


- ▲ High Risk Outfall
- SCP - Connecting Hwy*
- SCP - Local Program*
- SCP - State*
- Highway
- Connecting Highway (WisDOT 2024)
- TS4 Highway
- Urban Area (2020) and Municipalities with Population > 10k
- County Boundary
- Water

* Label indicates SCP_ID from the SCP Database
Permittee Name (C=City, T=Town, V=Village)

NE-2

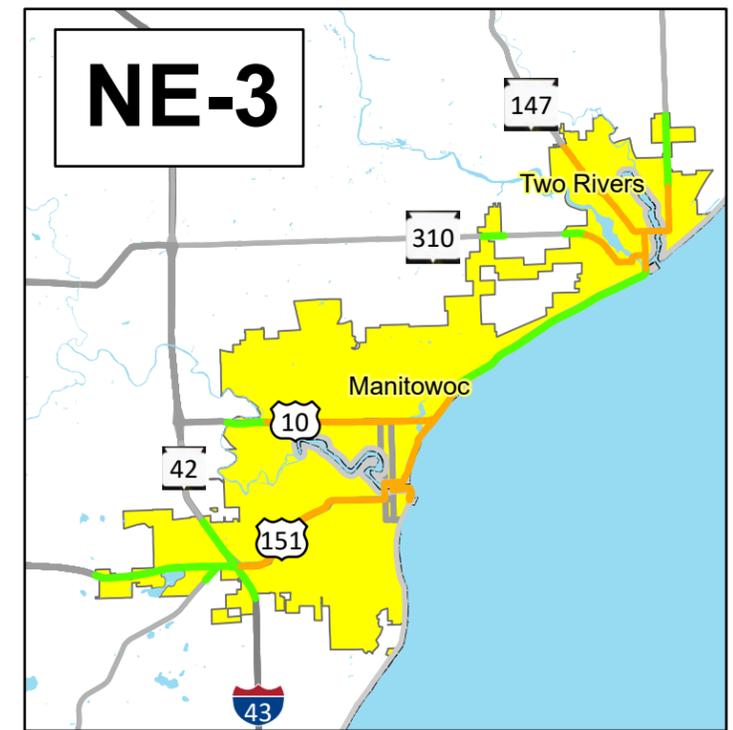




NE-3

- ▲ High Risk Outfall
- SCP - Connecting Hwy*
- SCP - Local Program*
- SCP - State*
- Highway
- Connecting Highway (WisDOT 2024)
- TS4 Highway
- Urban Area (2020) and Municipalities with Population > 10k
- County Boundary
- Water

* Label indicates SCP_ID from the SCP Database
Permittee Name (C=City, T=Town, V=Village)



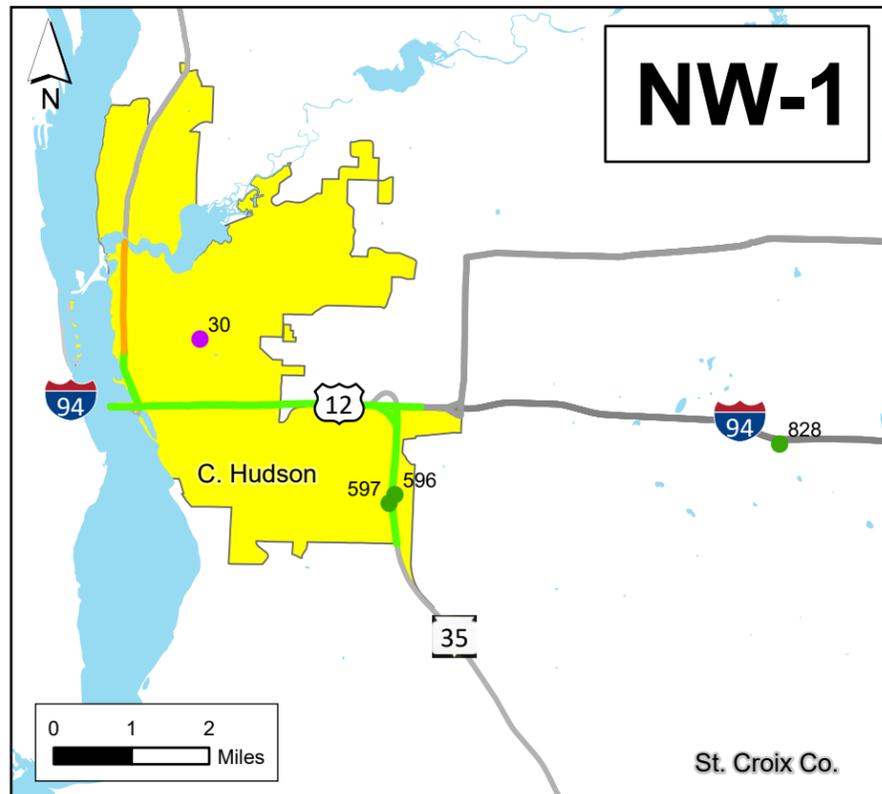
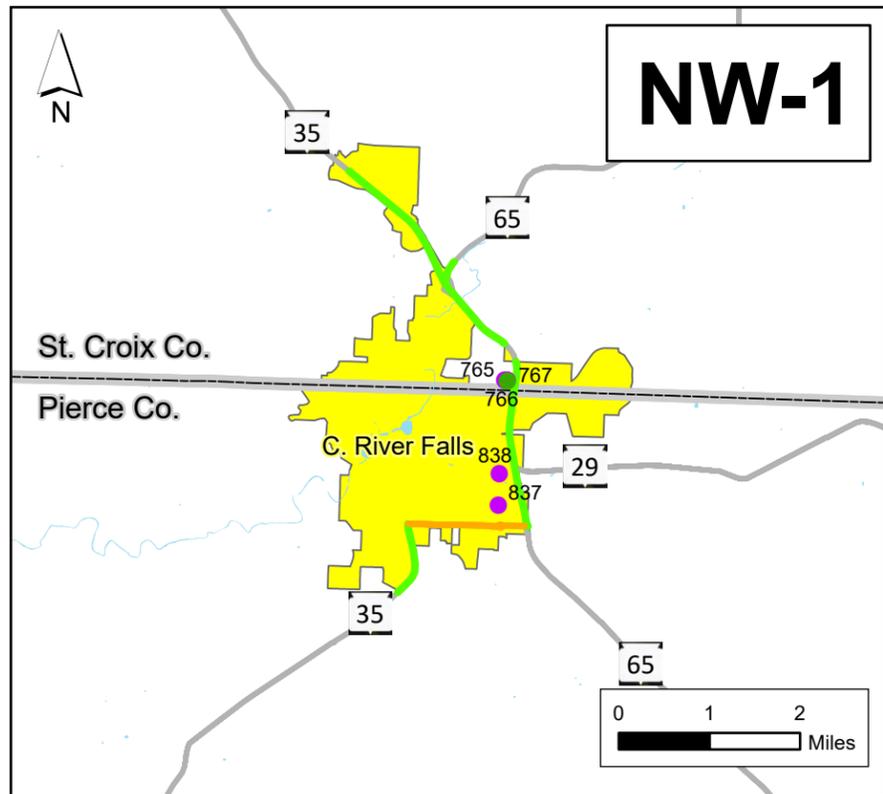
NE-3

08/25/2025

Statewide TS4 Mapping - 2024

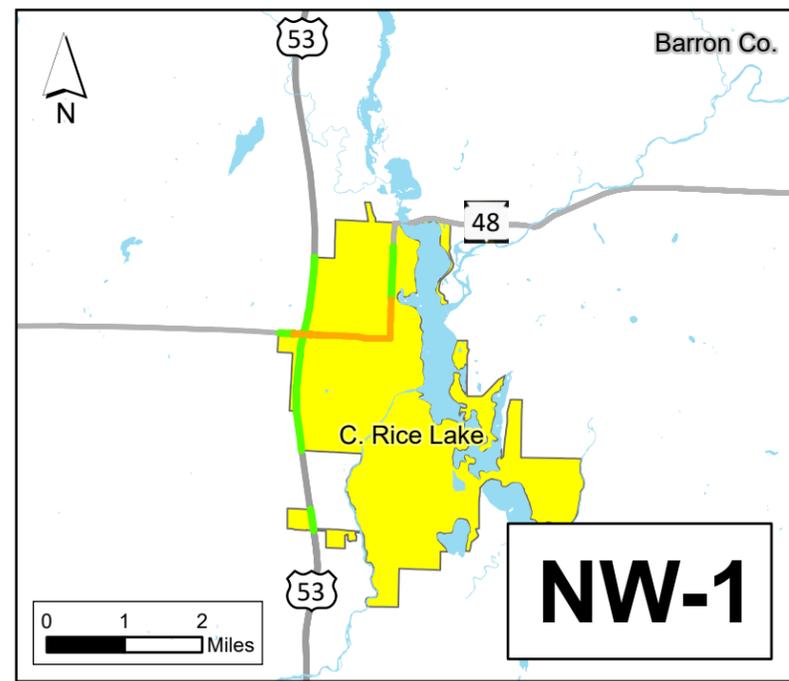
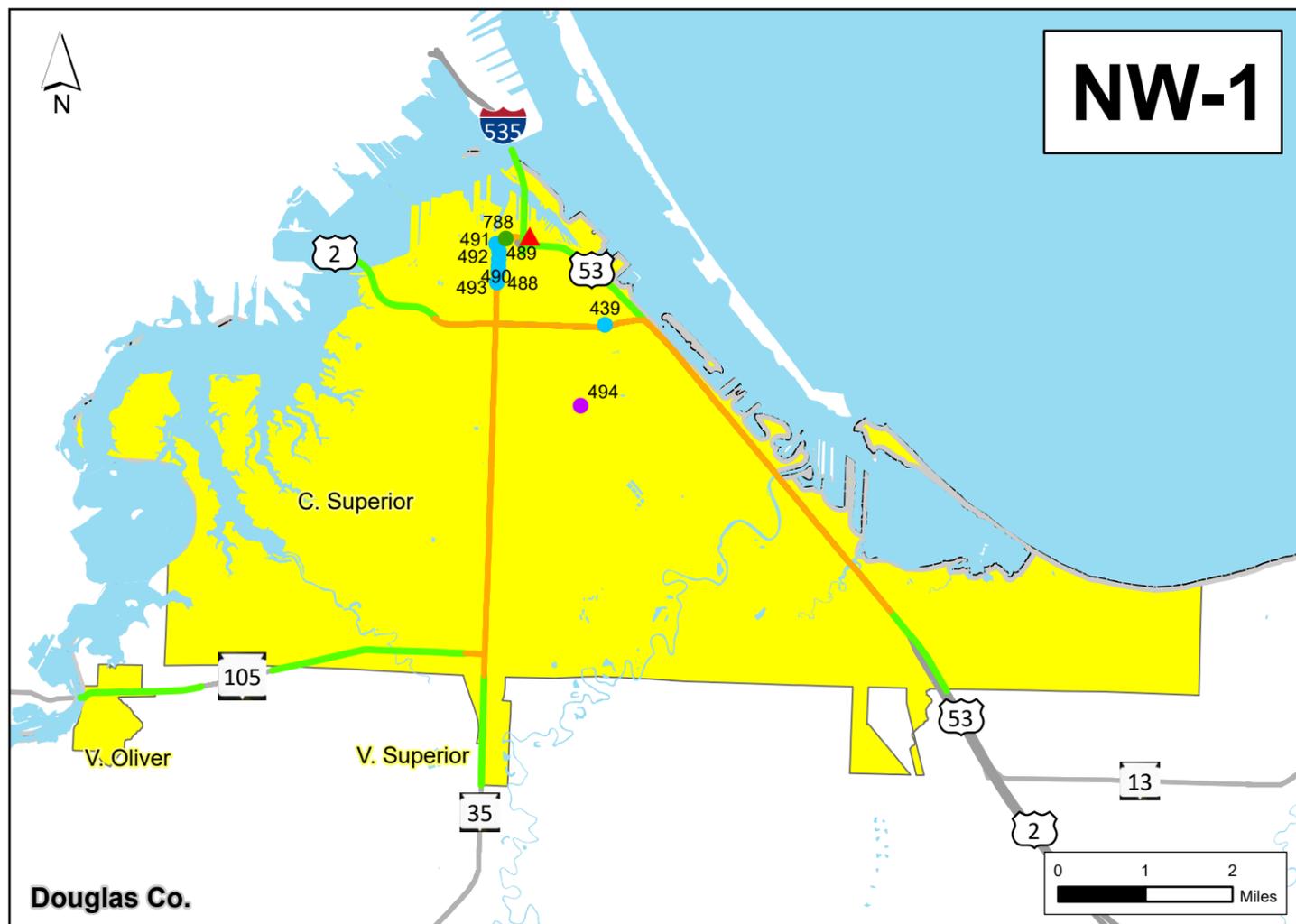


WisDOT Northeast Region

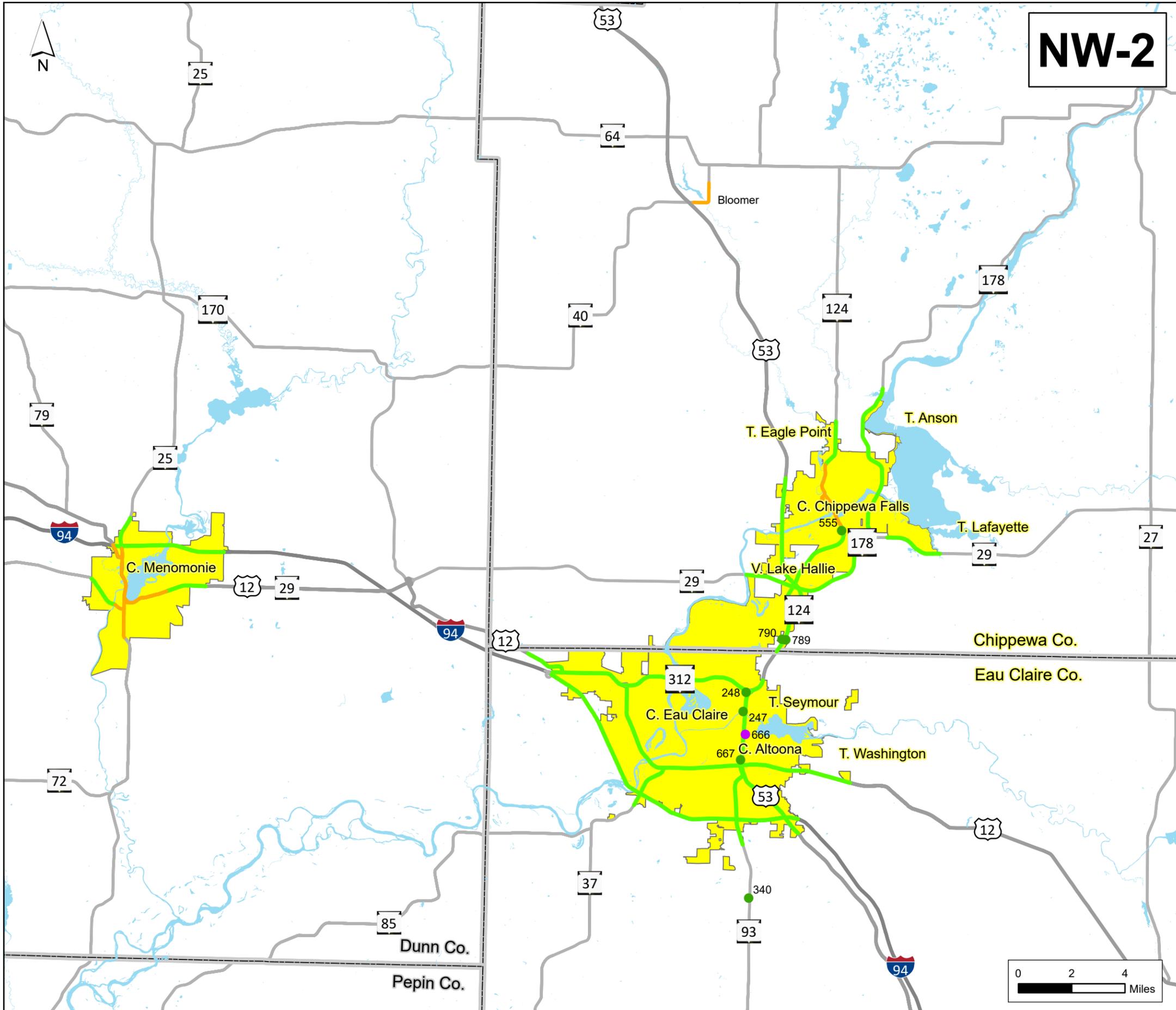


- ▲ High Risk Outfall
- SCP - Connecting Hwy*
- SCP - Local Program*
- SCP - State*
- Highway
- Connecting Highway (WisDOT 2024)
- TS4 Highway
- Urban Area (2020) and Municipalities with Population > 10k
- County Boundary
- Water

* Label indicates SCP_ID from the SCP Database
Permittee Name (C=City, T=Town, V=Village)



NW-2



- ▲ High Risk Outfall
- SCP - Connecting Hwy*
- SCP - Local Program*
- SCP - State*
- Highway
- Connecting Highway (WisDOT 2024)
- TS4 Highway
- Urban Area (2020) and Municipalities with Population > 10k
- ▭ County Boundary
- Water

* Label indicates SCP_ID from the SCP Database
Permittee Name (C=City, T=Town, V=Village)

08/25/2025

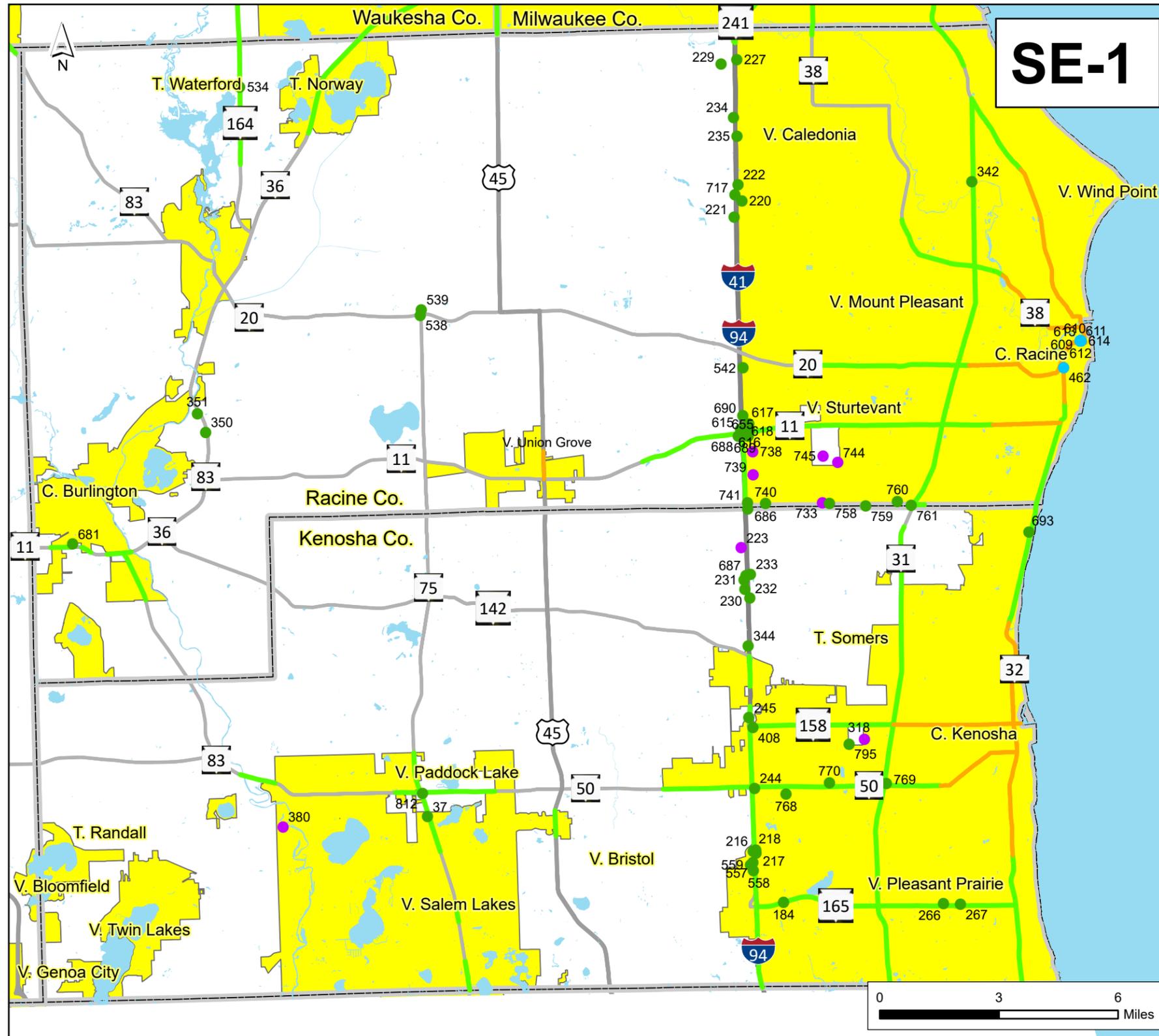
Statewide TS4 Mapping - 2024

WisDOT Northwest Region



0 2 4 Miles

Sheet NW-2

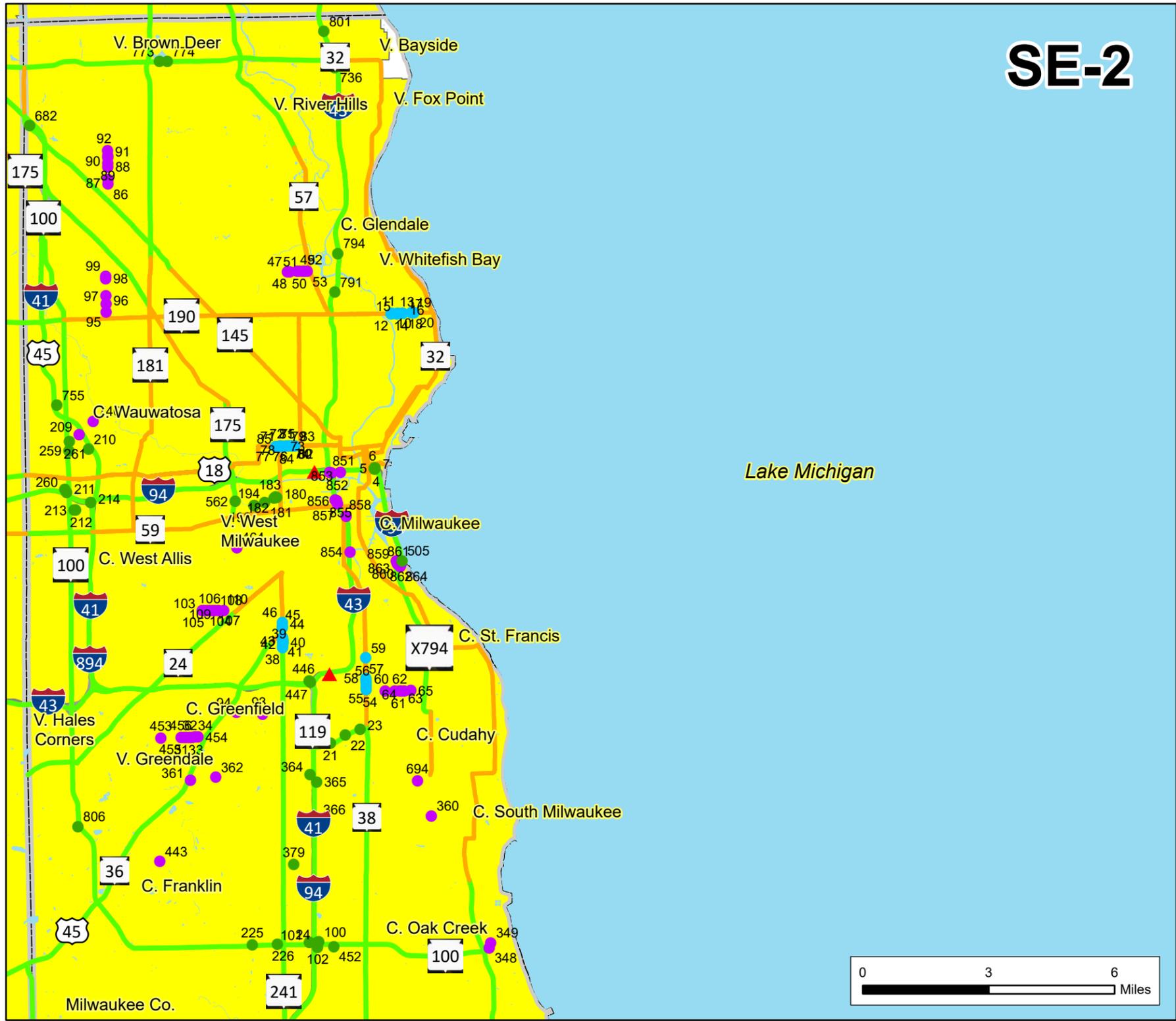


SE-1

- ▲ High Risk Outfall
- SCP - Connecting Hwy*
- SCP - Local Program*
- SCP - State*
- Highway
- Connecting Highway (WisDOT 2024)
- TS4 Highway
- Urban Area (2020) and Municipalities with Population > 10k
- County Boundary
- Water

* Label indicates SCP_ID from the SCP Database
Permittee Name (C=City, T=Town, V=Village)

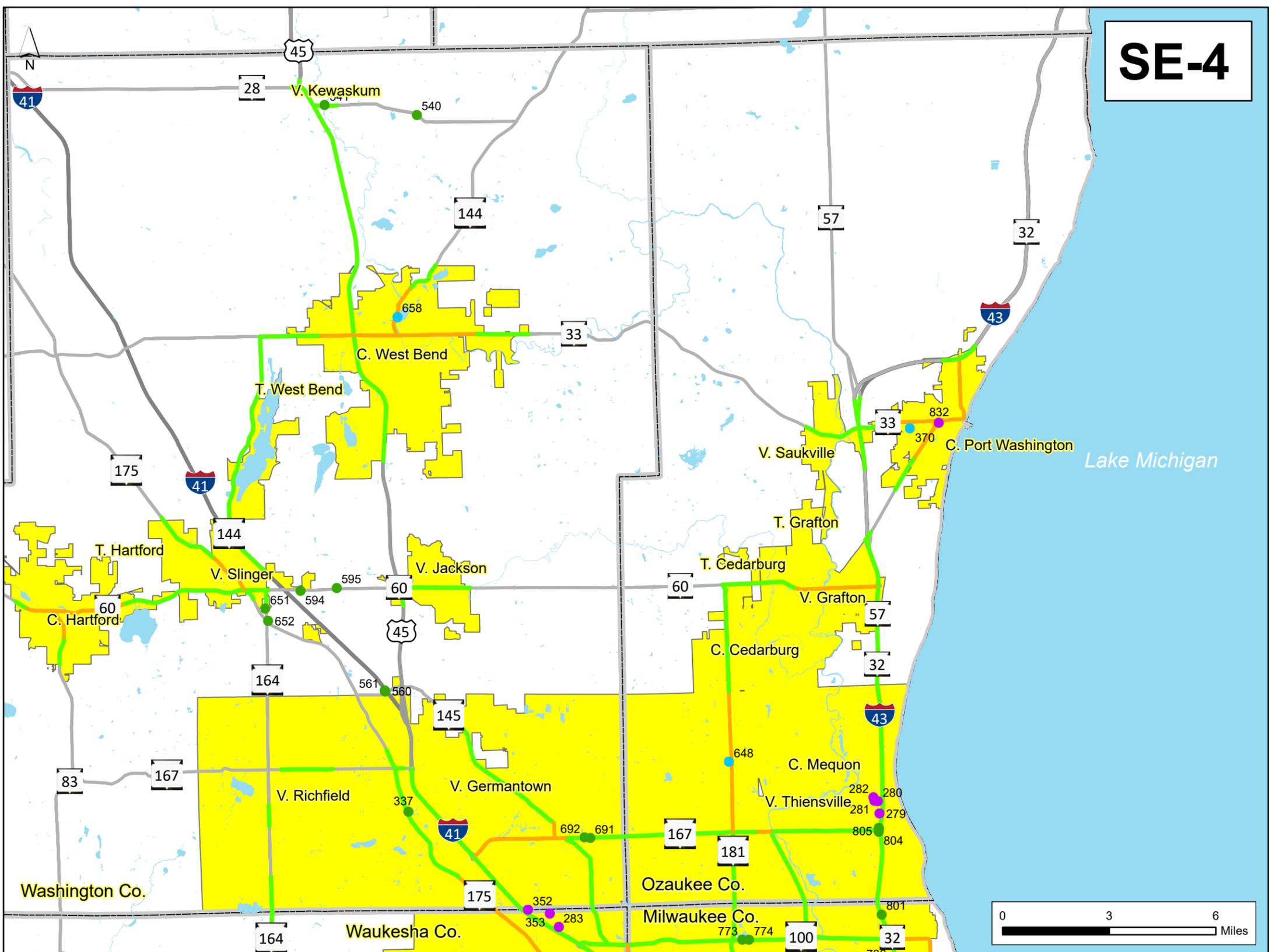
SE-2



- ▲ High Risk Outfall
- SCP - Connecting Hwy*
- SCP - Local Program*
- SCP - State*
- Highway
- Connecting Highway (WisDOT 2024)
- TS4 Highway
- Urban Area (2020) and Municipalities with Population > 10k
- County Boundary
- Water

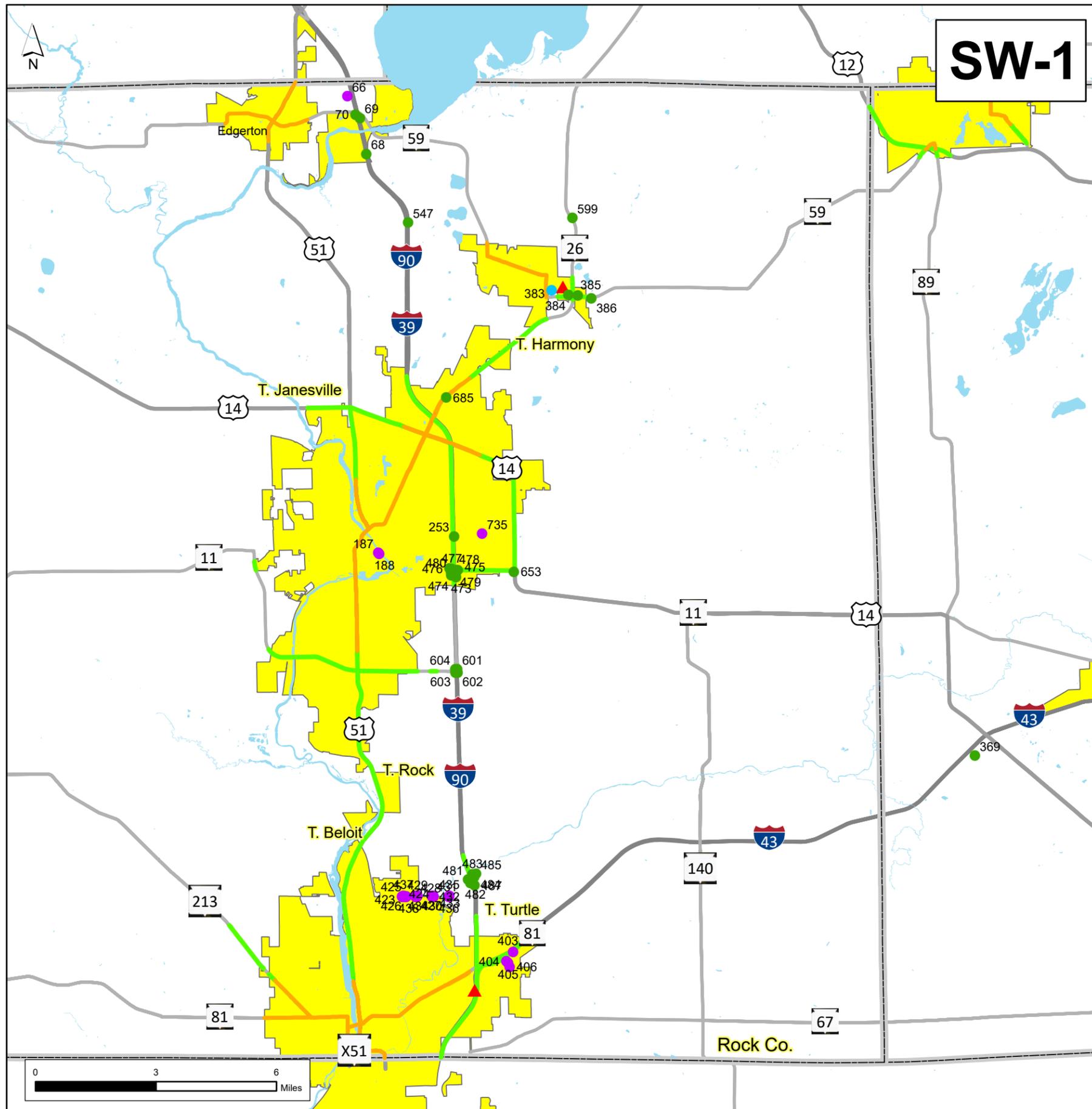
* Label indicates SCP_ID from the SCP Database
Permittee Name (C=City, T=Town, V=Village)

SE-4



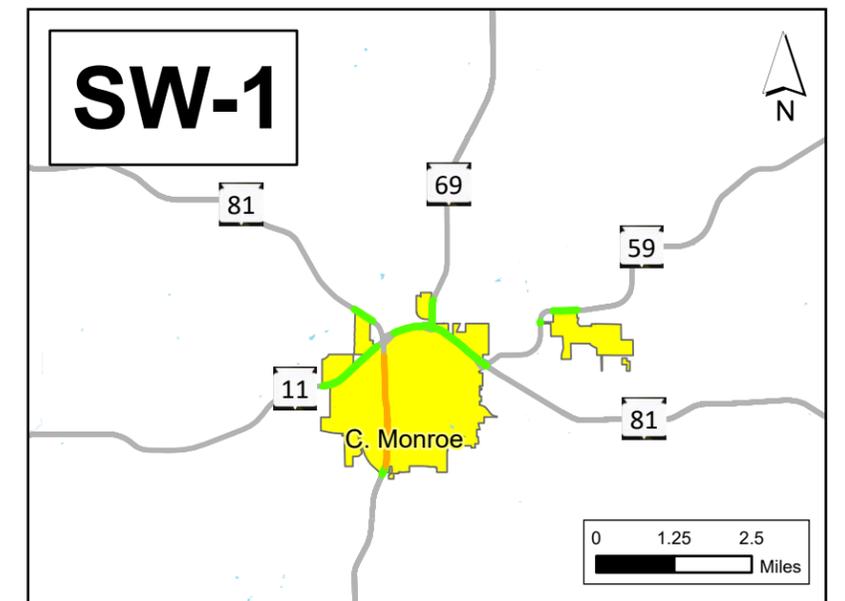
- ▲ High Risk Outfall
- SCP - Connecting Hwy*
- SCP - Local Program*
- SCP - State*
- Highway
- Connecting Highway (WisDOT 2024)
- TS4 Highway
- Urban Area (2020) and Municipalities with Population > 10k
- ▭ County Boundary
- Water

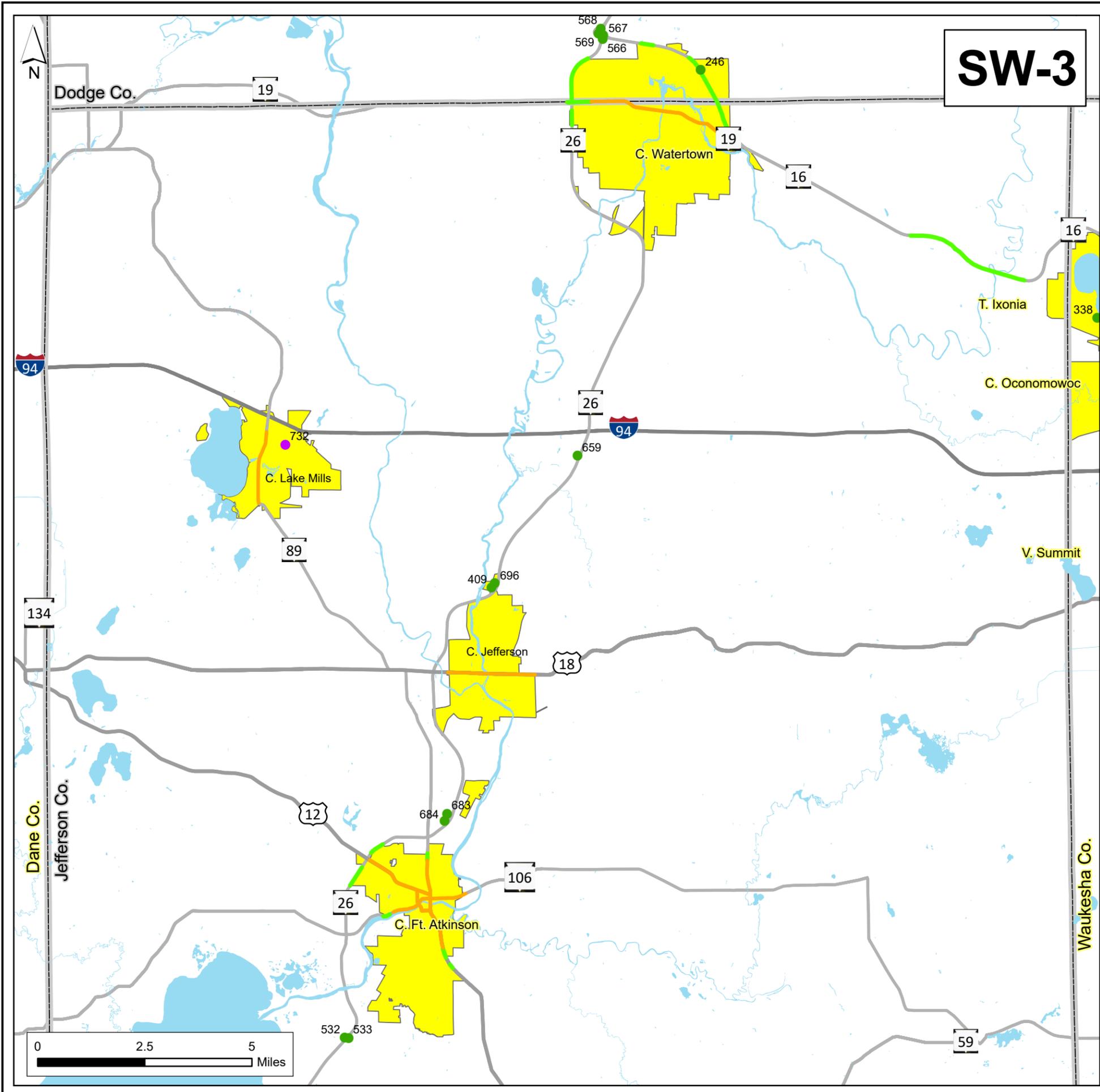
* Label indicates SCP_ID from the SCP Database
Permittee Name (C=City, T=Town, V=Village)



- ▲ High Risk Outfall
- SCP - Connecting Hwy*
- SCP - Local Program*
- SCP - State*
- Highway
- Connecting Highway (WisDOT 2024)
- TS4 Highway
- Urban Area (2020) and Municipalities with Population > 10k
- County Boundary
- Water

* Label indicates SCP_ID from the SCP Database
Permittee Name (C=City, T=Town, V=Village)

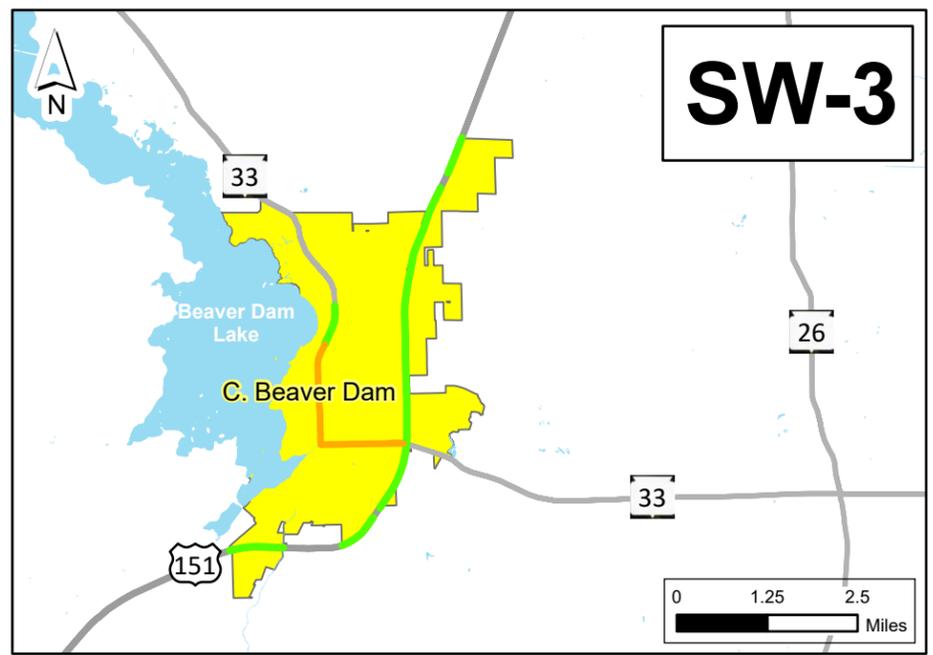




SW-3

- ▲ High Risk Outfall
- SCP - Connecting Hwy*
- SCP - Local Program*
- SCP - State*
- Highway
- Connecting Highway (WisDOT 2024)
- TS4 Highway
- Urban Area (2020) and Municipalities with Population > 10k
- County Boundary
- Water

* Label indicates SCP_ID from the SCP Database
Permittee Name (C=City, T=Town, V=Village)



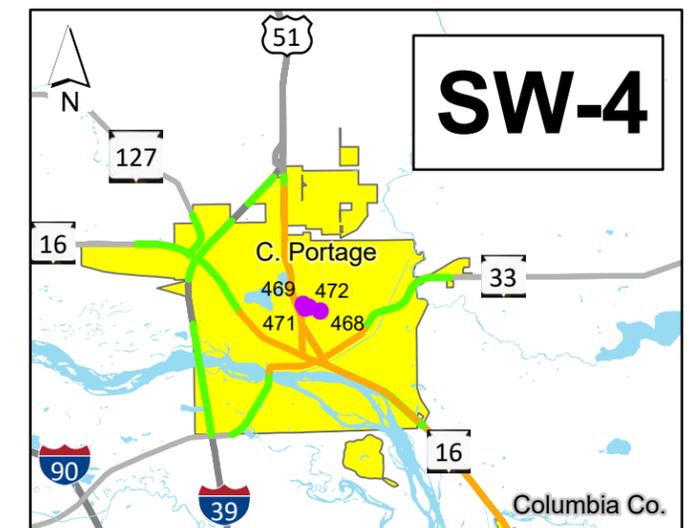
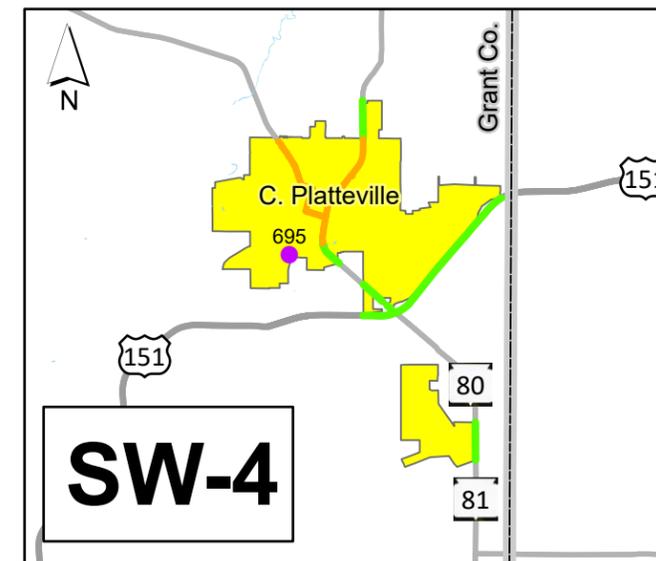
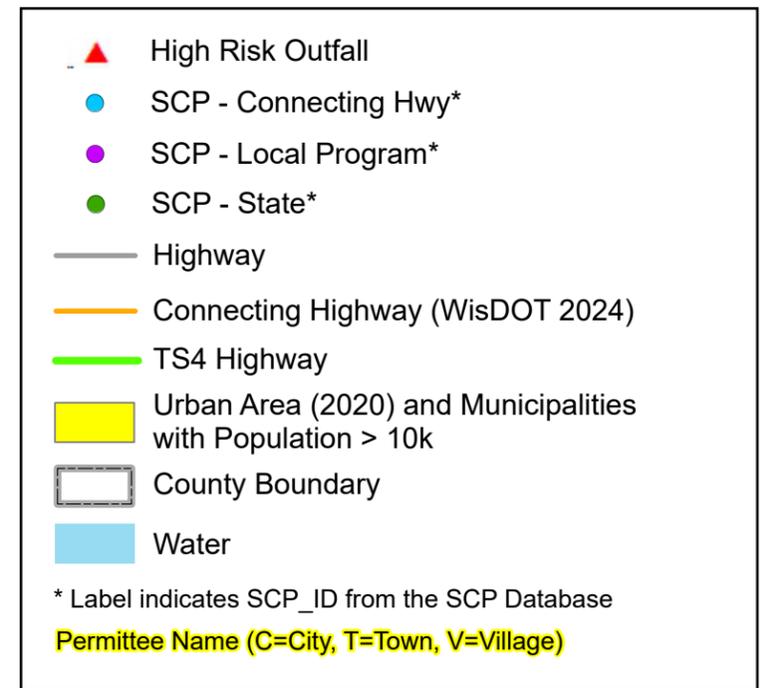
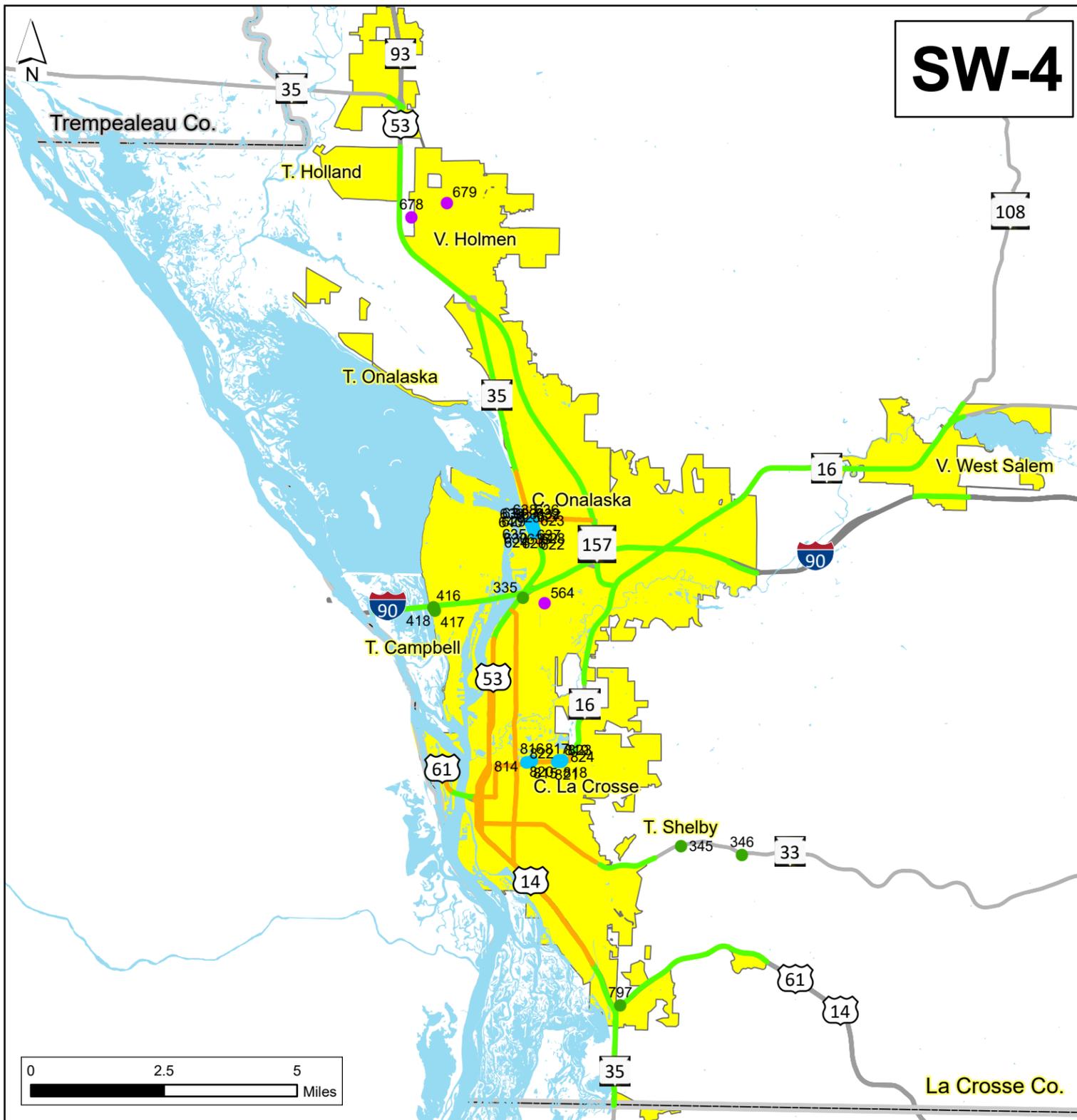
SW-3

08/25/2025

Statewide TS4 Mapping - 2024



WisDOT Southwest Region



Section E – Statewide Storm Sewer System Mapping Study Area Subbasins

See Section E in 2023 TS4 Permit Report:

[Wisconsin Department of Transportation Stormwater Management \(wisconsindot.gov\)](https://wisconsindot.gov)

Section F – SCP Inspection Forms and Maintenance Guidelines

WET DETENTION POND INSPECTION AND MAINTENANCE GUIDELINES

I. INSPECTIONS

- A. Inspections of the pond should be completed every 5 years. Also inspect ponds after a 5-inch rainfall in 24-hours to detect any potential structural failure problems.
- B. Inspection priorities shall be as follows:
 - 1. Inspect the embankments and pond sideslopes for settling/sinking, erosion, cracking, leakage/seepage, and animal holes.
 - 2. Inspect the condition of the emergency spillway and overland flow path downstream from the wet detention pond to the end of the WisDOT ROW and to stable ground if accessible.
 - 3. Inspect the pond for sediment accumulation. This may require a boat and solid material measuring device (surveyors level rod or marked stick) to compare to the as-built pond depths.
 - 4. Inspect the outlet control structure for clogging, debris accumulation and material failures.
 - 5. Inspect upstream and downstream channels, storm sewer and culvert outfalls for erosion.
 - 6. Inspect vegetation condition, noting any trees and/or woody plant growth on the inside, top and outside of the pond embankments.
 - 7. The inspector should be prepared to collect minor amounts of trash and debris as part of the inspection. It is recommended to bring along gloves and a trash bag. Large amounts of trash and debris would need to be noted for removal in the inspection report.
- C. As-built plans and an inspection report template shall accompany the inspector. The inspector should walk the entire facility paying attention to where stormwater enters/exits and any eroded or poorly vegetated areas. Take pictures at strategic locations to supplement inspection notes.
- D. Documentation of the inspections should be completed including:
 - 1. Inspector's name, affiliation and professional credentials.
 - 2. Date, time and weather conditions.
 - 3. Rainfall total over the previous 24-hour period.
 - 4. Erosion and/or sediment accumulation.
 - 5. Vegetation condition of inlet and outlet conveyances including the emergency spillway.
 - 6. Sediment depth at the outlet control structure and a minimum of one other location.
 - 7. Identification of potential structural failures and repair needs.

8. Other pond conditions such as algae growth and/or other water quality conditions.
 9. Repair recommendations.
- E. The inspection shall be filed with the regional Maintenance Department, the regional Stormwater & Erosion Control Engineer and the Statewide Stormwater Quality Engineer.

II. ROUTINE MAINTENANCE

A. Mowing

1. Side slopes, embankments, and emergency spillways that are not rock lined that have been planted with turf grasses should be mowed often enough to prevent woody growth and control noxious weeds.
2. It is recommended to mow native grasses to a height of 6" in mid to late summer or after they have achieved a height of 1-1/2 feet during the first growing season.

B. Debris and Litter Removal.

1. Complete debris and litter removal from the pond surface during or following each inspection.
2. Particular attention should be paid to debris accumulating around the outlet control structure to prevent potential clogging.

C. Erosion Control.

1. The pond sideslopes, embankments and emergency spillways may suffer from periodic slumping and erosion.
2. Corrective measures shall include re-grading, filling and re-vegetation/ seeding/mulching or erosion mat of the eroded or slumping areas.
3. Rip rap at the pond outlet and emergency spillways should be inspected for displacement or undermining. Make repairs when they are discovered.

D. Nuisance Control.

1. Biological control of algae and mosquitoes is preferred over chemical control. Consultation with local WDNR officials is recommended prior to the introduction of any biological control.
2. Maintaining a native grass or allowing the turf grass to get high along the water's edge perimeter will aide in geese control.
3. Use mechanical controls such as cutting or mowing the target nuisance/invasive species tight to the ground at strategic times or physical removal of both the plants and root structure when feasible.

III. NON-ROUTINE MAINTENANCE

A. Structural Repairs and/or Replacement

1. If any structural problems are noted of the inlet and outlet conveyance systems, embankment and/or emergency spillway the inspector should initiate repairs and/or replacement immediately.

B. Sediment Removal

1. Sediment clean out cycles of 15 to 25 years are typical. Sediment removal may be necessary prior to 10 years if there is a substantial amount of land disturbance occurring within the upstream watershed. Annual inspections shall be made to ensure that a minimum permanent water pool depth of at least 3.5' is maintained. When the depth is found to be less than this depth, schedule sediment removal.
2. The Department will develop a process to remove and dispose sediment that follows Chapter NR 528 of the Wisconsin Administrative Code.

WisDOT Stormwater Control Practice (SCP) Inspection Report Wet Detention Basin

Part A: Site Information

Region:	County:	Highway:
SCP ID:	SCP Description:	
State Project No.:	Weather:	
Inspector:	Inspection Date & Time:	
Inspector Firm/Region:	Inspector's email:	
24-hour Rainfall Total:	Last Rainfall End Date & Time:	
SCP Construction Year:	Date of Last Inspection:	
SCP Location:		

Part B: Maintenance Requirements

Summary of Urgent Maintenance Required: _____

Summary of Required Maintenance:

Assessment	Code	Location/Notes

Code Key			
N/A = Not Applicable	M = Monitor (potential for future problem)	NP = Not a Problem	WN = Work Needed

Part C: Detailed Inspection Notes

INFLOW POINTS

Assessment	Code	Comments
Obstruction: vegetation/debris/sediment		
Erosion/undercutting		
Displacement of fabric/rip rap		
Pipe Condition		
Other (describe)		

FOREBAY Circle one: Present/Not Present

Assessment	Code	Comments
Sediment depth (ft. below principal outlet)		
Side slope erosion		
Invasive vegetation		
Safety shelf		
Other (describe)		

MAIN POOL

Assessment	Code	Comments
Visible pollution/algae growth		
Sediment Depth (ft. below principal outlet)		
Vegetation height/type/location		
Bare soil/erosion		
Invasive vegetation (estimate a %)		
Weeds/algae (estimate a %)		
Pond Liner		
Safety Shelf		
Other (describe)		

EMBANKMENT

Assessment	Code	Comments
Erosion and/or loss of dam material		
Shrubs/trees present		
Animal burrows		
Soft spots/settlement/cracking/seepage		
Emergency spillway to ROW		
Other (describe)		

OUTLET DEVICE

Assessment	Code	Comments
Obstruction: vegetation/debris/sediment		
Erosion/undercutting		
Joint failure/loss of joint material		
Outfall riprap/scour prevention		
Other (describe)		

MISCELLANEOUS

Assessment	Code	Comments
Trash/debris		
Access		
Vandalism		
Fence condition (if applicable)		
Wildlife observations		
Signage (if applicable)		
Sediment disposal site (available?)		
Other (describe)		

ADDITIONAL COMMENTS:

1. N/A

MAINTENANCE/REPAIR RECOMMENDATIONS:

1. N/A

INSTRUCTIONS FOR SCP INSPECTION FORM:

1. Complete Part A: Site Information
2. Complete Part C: Detailed Inspection Notes
3. Complete Part B: Maintenance Requirements
4. Attach color digital photographs of the site and structural SCPs using a photo log.
5. Update inspection spreadsheet

DRY DETENTION POND INSPECTION AND MAINTENANCE GUIDELINES

I. INSPECTIONS

- A. Inspections of the pond should be completed every 5 years. Also inspect ponds after a 5-inch rainfall in 24-hours to detect any potential structural failure problems.

- B. Inspection priorities shall be as follows:
 - 1. Inspect the embankments and pond sideslopes for settling/sinking, erosion, cracking, leakage/seepage, and animal holes.
 - 2. Inspect the condition of the emergency spillway and overland flow path downstream from the dry detention pond to the end of the WisDOT ROW and to stable ground if accessible.
 - 3. Inspect the pond for sediment accumulation. This may require the installation of marked elevation markers to allow inspectors to check sediment levels over time.
 - 4. Inspect the outlet control structure for clogging, debris accumulation and material failures.
 - 5. Inspect upstream and downstream channels, storm sewer and culvert outfalls for erosion.
 - 6. Inspect vegetation condition, noting any trees and/or woody plant growth on the inside, top and outside of the pond embankments.
 - 7. The inspector should be prepared to collect minor amounts of trash and debris as part of the inspection. It is recommended to bring along gloves and a trash bag. Large amounts of trash and debris would need to be noted for removal in the inspection report.

- C. As-built plans and an inspection report template shall accompany the inspector. The inspector should walk the entire facility paying attention to where stormwater enters/exits and any eroded or poorly vegetated areas. Take pictures at strategic locations to supplement inspection notes.

- D. Documentation of the inspections should be completed including:
 - 1. Inspector's name, affiliation and professional credentials.
 - 2. Date, time and weather conditions.
 - 3. Rainfall total over the previous 24-hour period.
 - 4. Erosion and/or sediment accumulation.
 - 5. Vegetation condition of inlet and outlet conveyances including emergency spillway.
 - 6. Identification of potential structural failures and repair needs.
 - 7. Repair recommendations.

- E. The inspection shall be filed with the regional Maintenance Department, the regional Stormwater & Erosion Control Engineer and the Statewide Stormwater Quality Engineer.

II. ROUTINE MAINTENANCE

A. Mowing

1. Side slopes, embankments, and emergency spillways that are not rock lined which have been planted with turf grasses should be mowed often enough to prevent woody growth and control noxious weeds.
2. It is recommended to mow native grasses to a height of 6" in mid to late summer or after they have achieved a height of 1-1/2 feet during the first growing season.

B. Debris and Litter Removal.

1. Debris and litter removal from the pond surface shall be completed during or following each inspection.
2. Pay particular attention to debris accumulating around the outlet control structure to prevent potential clogging.

C. Erosion Control.

1. The pond sideslopes, embankments and emergency spillways may suffer from periodic slumping and erosion.
2. Corrective measures shall include re-grading, filling and re-vegetation/seeding/mulching or erosion mat of the eroded or slumping areas.
3. Rip rap at the pond outlet and emergency spillways should be inspected for displacement or undermining. Make repairs when they are discovered.

III. NON-ROUTINE MAINTENANCE

A. Structural Repairs and/or Replacement

1. If any structural problems are noted of the inlet and outlet conveyance systems, embankment and/or emergency spillway the inspector should initiate repairs and/or replacement immediately.

WisDOT Stormwater Control Practice (SCP) Inspection Report Dry Detention Basin

Part A: Site Information

Region:	County:	Highway:
SCP ID:	SCP Description:	
State Project No.:	Weather:	
Inspector:	Inspection Date & Time:	
Inspector Firm/Region:	Inspector's email:	
24-hour Rainfall Total:	Last Rainfall End Date & Time:	
SCP Construction Year:	Date of Last Inspection:	
SCP Location:		

Part B: Maintenance Requirements

Summary of Urgent Maintenance Required: _____

Summary of Required Maintenance:

Assessment	Code	Location/Notes

Code Key			
N/A = Not Applicable	M = Monitor (potential for future problem)	NP = Not a Problem	WN = Work Needed

Part C: Detailed Inspection Notes

INFLOW POINTS

Assessment	Code	Comments
Obstruction: vegetation/debris/sediment		
Erosion/undercutting		
Displacement of fabric/rip rap		
Pipe Condition		
Other (describe)		

FOREBAY

Assessment	Code	Comments
Sediment depth (ft. below principal outlet)		
Side slope erosion		
Invasive vegetation		
Safety shelf		
Other (describe)		

MAIN BASIN AREA

Assessment	Code	Comments
Visible pollution/algae growth		
Sediment accumulation		
Vegetation height/type/location		
Bare soil/erosion		
Invasive vegetation (estimate a %)		
Weeds (estimate a %)		
Other (describe)		

EMBANKMENT

Assessment	Code	Comments
Erosion and/or loss of dam material		
Shrubs/trees present		
Animal burrows		
Soft spots/settlement/cracking/seepage		
Emergency spillway to ROW		
Other (describe)		

OUTLET DEVICE

Assessment	Code	Comments
Obstruction: vegetation/debris/sediment		
Erosion/undercutting		
Joint failure/loss of joint material		
Outfall riprap/scour prevention		
Other (describe)		

MISCELLANEOUS

Assessment	Code	Comments
Trash/debris		
Access		
Vandalism		
Fence condition (if applicable)		
Wildlife observations		
Signage (if applicable)		
Sediment disposal site (available?)		
Other (describe)		

ADDITIONAL COMMENTS:

1. N/A

MAINTENANCE/REPAIR RECOMMENDATIONS:

1. N/A

INSTRUCTIONS FOR SCP INSPECTION FORM:

1. Complete Part A: Site Information
2. Complete Part C: Detailed Inspection Notes
3. Complete Part B: Maintenance Requirements
4. Attach color digital photographs of the site and structural SCPs using a photo log.
5. Update inspection spreadsheet

INFILTRATION BASIN INSPECTION AND MAINTENANCE GUIDELINES

I. INSPECTION

- A. Inspections of the basin should be completed every 5 years. Also inspect basins after a 5-inch rainfall in 24-hours to detect any potential structural failure problems. Typically the construction project will include infiltration basin maintenance during the vegetation establishment period.

- B. Inspection priorities shall be as follows:
 - 1. Inspect the embankments and basin sideslopes for settling/sinking, erosion, cracking, leakage/seepage, and animal holes.
 - 2. Inspect the condition of the inlets and outlets and any level spreaders or emergency spillways downstream from the infiltration basin to the end of the WisDOT ROW and to stable ground if accessible.
 - 3. Inspect the basin for accumulation of sediment. This may require the installation of marked elevation markers to allow inspectors to check sediment levels over time.
 - 4. Inspect the outlet control structure for clogging, debris accumulation and material failures.
 - 5. Inspect upstream and downstream channels, storm sewer and culvert outfalls for erosion.
 - 6. Inspect vegetation condition, noting any trees and/or woody plant growth on the inside, top and outside of the basin embankments.
 - 7. Identify whether water ponds in over 50% of the basin for more 3 days following a rainfall.
 - 8. The inspector should be prepared to collect minor amounts of trash and debris as part of the inspection. It is recommended to bring along gloves and a trash bag. Large amounts of trash and debris would need to be noted for removal in the inspection report.

- C. As-built plans and an inspection report template shall accompany the inspector. The inspector should walk the entire facility paying attention to where stormwater enters/exits and any eroded or poorly vegetated areas. Take pictures at strategic locations to supplement inspection notes.

- D. Documentation of the inspections should be completed including:
 - 1. Inspector's name, affiliation and professional credentials.
 - 2. Date, time and weather conditions.
 - 3. Rainfall total over the previous 24-hour period.
 - 4. Erosion and/or sediment accumulation.
 - 5. Vegetation condition of inlet and outlet conveyances including emergency spillway.
 - 6. Identification of potential structural failures and repair needs.
Repair recommendations.

- E. The inspection shall be filed with the regional Maintenance Department, the regional Stormwater & Erosion Control Engineer and the Statewide Stormwater Quality Engineer

II. ROUTINE MAINTENANCE

- A. Mowing – Native Vegetation
 1. During establishment of vegetation, the first mowing shall occur once it reaches a height of 10 to 12 inches.
 2. Control woody plant invasion by mowing or routine burning as necessary. The vegetation height shall be 5 to 6 inches after mowing.
 3. If burning cannot be accommodated, mow once per year in the fall after November 1st.
 4. Remove trash and debris at the time of mowing.

III. NON-ROUTINE MAINTENANCE

- A. Structural Repairs and/or Replacement
 1. If any structural problems are noted of the inlet and outlet conveyance systems, embankment and/or emergency spillway the inspector should initiate repairs and/or replacement immediately.
- B. Restoration of Infiltrative Capacity
 1. Over time the original infiltrative capacity of the infiltration basin may diminish.
 2. The inspections shall include an evaluation of drawdown time. If water is ponding more than 72 hours after rainfall stops entering the infiltration basin, the infiltrative capacity needs to be restored. The inspector shall initiate restoration of the infiltration basin soil profile per the original plans unless otherwise approved.
- C. Deep Tilling/Subsoiling
 1. If the basin is ponding more than 72 hours, the inspector should identify whether deep tilling or subsoiling would be sufficient to maintain infiltration capacity and initiate the appropriate restoration of infiltrative capacity.
 2. Deep tilled and subsoiled areas should be immediately re-vegetated to prevent erosion and scour.
- D. Full Removal and Replacement of Soil Profile
 1. If the inspector determines that deep tilling or subsoiling would not be sufficient to maintain infiltration capacity, full removal and replacement of the existing soil profile to permeable soils should be initiated to restore the infiltrative capacity. The replacement soils shall match the original design unless otherwise approved.

2. Immediately re-vegetate and stabilize disturbed areas to prevent erosion and sedimentation.

WisDOT Stormwater Control Practice (SCP) Inspection Report Infiltration Basin

Part A: Site Information

Region: Southeast	County:	Highway
SCP ID:	SCP Description:	
State Project No.:	Weather:	
Inspector:	Inspection Date & Time:	
Inspector Firm/Region:	Inspector's email:	
24-hour Rainfall Total:	Last Rainfall End Date & Time:	
SCP Construction Year:	Date of Last Inspection:	
SCP Location:		

Part B: Maintenance Requirements

Summary of Urgent Maintenance Required: _____

Summary of Required Maintenance:

Assessment	Code	Location/Notes

Code Key			
N/A = Not Applicable	M = Monitor (potential for future problem)	NP = Not a Problem	WN = Work Needed

Part C: Detailed Inspection Notes

INFLOW POINTS

Assessment	Code	Comments
Obstruction: vegetation/debris/sediment		
Erosion/undercutting		
Displacement of fabric/rip rap		
Pipe Condition		
Other (describe)		

MAIN BASIN AREA

Assessment	Code	Comments
Standing water (> 3 days after storm) + % cover		
Sediment accumulation		
Vegetation height/type		
Bare soil/erosion		
Invasive vegetation (estimate a %)		
Weeds (estimate a %)		
Infiltration trench		
Cell dividers/level spreaders		
Other (describe)		

EMBANKMENT

Assessment	Code	Comments
Erosion and/or loss of dam material		
Shrubs/trees present		
Animal burrows		
Soft spots or settlement		
Emergency spillway		
Other (describe)		

OUTLET DEVICE

Assessment	Code	Comments
Obstruction: vegetation/debris/sediment		
Erosion/undercutting		
Joint failure/loss of joint material		
Outfall riprap/scour prevention		
Other (describe)		

MISCELLANEOUS

Assessment	Code	Comments
Trash/debris		
Access		
Vandalism		
Fence condition (if applicable)		
Wildlife observations		
Signage (if applicable)		
Sediment disposal site (available?)		
Other (describe)		

ADDITIONAL COMMENTS:

1. N/A

MAINTENANCE/REPAIR RECOMMENDATIONS:

1. N/A

INSTRUCTIONS FOR SCP INSPECTION FORM:

1. Complete Part A: Site Information
2. Complete Part C: Detailed Inspection Notes
3. Complete Part B: Maintenance Requirements
4. Attach color digital photographs of the site and structural SCPs using a photo log.
5. Update inspection spreadsheet

BIORETENTION BASIN INSPECTION AND MAINTENANCE GUIDELINES

I. INSPECTION

- A. Inspections of the basin shall be completed quarterly for the first year after construction or reconstruction. Also inspect basins after a 5-inch rainfall in 24-hours to detect any potential structural failure problems. Typically the construction project will include bioretention basin maintenance during the vegetation establishment period.

- B. Inspection priorities shall be as follows:
 - 1. Inspect the embankments and basin sideslopes for settling/sinking, erosion, cracking, leakage/seepage, and animal holes.
 - 2. Inspect the condition of the inlets and outlets and any level spreaders or emergency spillways downstream from the basin to the end of the WisDOT ROW and to stable ground if accessible
 - 3. Inspect the basin for sediment accumulation. This may require the installation of marked elevation markers to allow inspectors to check sediment levels over time.
 - 4. Inspect the outlet control structure for clogging, debris accumulation and material failures.
 - 5. Inspect upstream and downstream channels, storm sewer and culvert outfalls for erosion.
 - 6. Inspect vegetation condition, noting any trees and/or woody plant growth on the inside, top and outside of the basin embankments.
 - 7. The inspector should be prepared to collect minor amounts of trash and debris as part of the inspection. It is recommended to bring along gloves and a trash bag. Large amounts of trash and debris would need to be noted for removal in the inspection report.
 - 8. Inspect for any signs of drain tile failure such as surface soil depressions indicating drain tile line breaks.
 - 9. Identify whether water ponds in over 50% of the basin for more 3 days following a rainfall.

- C. As-built plans and an inspection report template shall accompany the inspector. The inspector should walk the entire facility paying attention to where stormwater enters/exits and any eroded or poorly vegetated areas. Take pictures at strategic locations to supplement inspection notes.

- D. Documentation of the inspections should be completed including:
 - 1. Inspector's name, affiliation and professional credentials.
 - 2. Date, time and weather conditions.
 - 3. Rainfall total over the previous 24-hour period.
 - 4. Erosion and/or sediment accumulation.

5. Vegetation condition of inlet and outlet conveyances including emergency spillway.
 6. Identification of potential structural failures and repair needs. Repair recommendations.
- E. The inspection shall be filed with the regional Maintenance Department, the regional Stormwater & Erosion Control Engineer and the Statewide Stormwater Quality Engineer.

II. ROUTINE MAINTENANCE

- A. Mowing – Native Vegetation
1. During establishment of vegetation, the first mowing shall occur once it reaches a height of 10 to 12 inches. Typically the construction project will include bioretention basin maintenance during the vegetation establishment period.
 2. Control woody plant invasion by mowing or routine burning as necessary. The vegetation height shall be 5 to 6 inches after mowing.
 3. If burning cannot be accommodated, mow once per year in the fall after November 1st.
 4. Remove trash and debris at the time of mowing.

III. NON-ROUTINE MAINTENANCE

- A. Structural Repairs and/or Replacement
1. If any structural problems are noted of the inlet and outlet conveyance systems, embankment and/or emergency spillway the inspector should initiate repairs and/or replacement immediately.
- B. Restoration of Infiltrative Capacity
1. Over time the original infiltrative capacity of the bioretention basin may diminish.
 2. The inspections shall include an evaluation of drawdown time. If water is ponding more than 72 hours after rainfall stops entering the bioretention basin, the infiltrative capacity needs to be restored. The inspector shall initiate restoration of the bioretention basin soil profile per the original plans unless otherwise approved.
- C. Deep Tilling/Subsoiling
1. If the basin is ponding more than 72 hours, the inspector should identify whether deep tilling or subsoiling would be sufficient to maintain infiltration capacity and initiate the appropriate restoration of infiltrative capacity.
 2. Deep tilled and subsoiled areas should be immediately re-vegetated to prevent erosion.

D. Full Removal and Replacement of Soil Profile

1. If the inspector determines that deep tilling or subsoiling would not be sufficient to maintain infiltration capacity, full removal and replacement of the existing soil profile to permeable soils should be initiated to restore the infiltrative capacity. The replacement soils shall match the original design unless otherwise approved.
2. Disturbed areas should be immediately re-vegetated and stabilized to prevent erosion and sedimentation.

WisDOT Stormwater Control Practice (SCP) Inspection Report Bioretention Device

Part A: Site Information

Region:	County:	Highway:
SCP ID:	SCP Description:	
State Project No.:	Weather:	
Inspector:	Inspection Date & Time:	
Inspector Firm/Region:	Inspector's email:	
24-hour Rainfall Total:	Last Rainfall End Date & Time:	
SCP Construction Year:	Date of Last Inspection:	
SCP Location:		

Part B: Maintenance Requirements

Summary of Urgent Maintenance Required: _____

Summary of Required Maintenance:

Assessment	Code	Location/Notes

Code Key			
N/A = Not Applicable	M = Monitor (potential for future problem)	NP = Not a Problem	WN = Work Needed

Part C: Detailed Inspection Notes

INFLOW POINTS

Assessment	Code	Comments
Obstruction: vegetation/debris/sediment		
Erosion/undercutting		
Displacement of fabric/rip rap		
Pipe/inlet structure Condition		
Other (describe)		

MAIN BASIN AREA

Assessment	Code	Comments
Standing water (> 3 days after storm) + % cover		
Sediment accumulation		
Vegetation height/type		
Bare soil/erosion		
Invasive vegetation (estimate a %)		

Weeds (estimate a %)		
Surface soil depressions, holes or cracking		
Cell dividers/level spreaders		
Infiltration rate test, if applicable		
Emergency sump condition		
Other (describe)		

EMBANKMENT

Assessment	Code	Comments
Erosion and/or loss of dam material		
Shrubs/trees present		
Animal burrows		
Soft spots or settlement		
Emergency spillway condition		
Other (describe)		

OUTLET DEVICE

Assessment	Code	Comments
Obstructions: vegetation/debris/sediment		
Erosion/undercutting		
Joint failure/loss of joint material		
Outfall riprap/scour prevention		
Drain tile obstructions		
Other (describe)		

MISCELLANEOUS

Assessment	Code	Comments
Trash/debris		
Basin access route condition		
Vandalism		
Fence condition (if applicable)		
Wildlife observations		
Signage (if applicable)		
Sediment disposal site (available?)		
Sediment/water staff gage condition		
Other (describe)		

ADDITIONAL COMMENTS:

1. N/A

MAINTENANCE/REPAIR RECOMMENDATIONS:

1. N/A

INSTRUCTIONS FOR SCP INSPECTION FORM:

1. Complete Part A: Site Information
2. Complete Part C: Detailed Inspection Notes
3. Complete Part B: Maintenance Requirements
4. Attach color digital photographs of the site and structural SCPs using a photo log.
5. Update inspection spreadsheet

WisDOT Stormwater Control Practice (SCP) Inspection Report Rain Garden

Part A: Site Information

Region	County	Highway
SCP ID:		SCP Description:
State Project No.:		Weather:
Inspector:		Inspection Date & Time:
Inspector Firm/Region:		Inspector's email:
24-hour Rainfall Total:		Last Rainfall End Date & Time:
SCP Construction Year:		Date of Last Inspection:
SCP Location:		

Part B: Maintenance Requirements

Summary of Urgent Maintenance Required: _____

Summary of Required Maintenance:

Assessment	Code	Location/Notes

Code Key			
N/A = Not Applicable	M = Monitor (potential for future problem)	NP = Not a Problem	WN = Work Needed

Part C: Detailed Inspection Notes

INFLOW POINTS

Assessment	Code	Comments
Obstruction: vegetation/debris/sediment		
Erosion/undercutting		
Displacement of fabric/rip rap		
Pipe Condition		
Other (describe)		

MAIN BASIN AREA

Assessment	Code	Comments
Standing water (> 3 days after storm) + % cover		
Sediment accumulation		
Vegetation height/type		
Bare soil/erosion		
Invasive vegetation (estimate a %)		

Weeds (estimate a %)		
Infiltration trench		
Cell dividers/level spreaders		
Other (describe)		

EMBANKMENT

Assessment	Code	Comments
Erosion and/or loss of dam material		
Shrubs/trees present		
Animal burrows		
Soft spots or settlement		
Emergency spillway		
Other (describe)		

OUTLET DEVICE

Assessment	Code	Comments
Obstruction: vegetation/debris/sediment		
Erosion/undercutting		
Joint failure/loss of joint material		
Outfall riprap/scour prevention		
Other (describe)		

MISCELLANEOUS

Assessment	Code	Comments
Trash/debris		
Access		
Vandalism		
Fence condition (if applicable)		
Wildlife observations		
Signage (if applicable)		
Sediment disposal site (available?)		
Other (describe)		

ADDITIONAL COMMENTS:

1. N/A

MAINTENANCE/REPAIR RECOMMENDATIONS:

1. N/A

INSTRUCTIONS FOR SCP INSPECTION FORM:

1. Complete Part A: Site Information
2. Complete Part C: Detailed Inspection Notes
3. Complete Part B: Maintenance Requirements
4. Attach color digital photographs of the site and structural SCPs using a photo log.
5. Update inspection spreadsheet

INFILTRATION TRENCH INSPECTION AND MAINTENANCE GUIDELINES

I. INSPECTION

- A. Inspections of the trench should be completed every 5 years. Also inspect trenches after a 5-inch rainfall in 24-hours to detect any potential structural failure problems.
- B. Inspection priorities shall be as follows:
 - 1. Inspect the trench surface for accumulation of sediment and debris.
 - 2. Inspect upstream and downstream channels for erosion.
 - 3. Inspect vegetation condition, noting any trees and/or woody plant growth. Include condition of pretreatment areas.
 - 4. Identify whether water ponds in over 50% of the trench for more 3 days following a rainfall.
- C. As-built plans and an inspection report template shall accompany the inspector. The inspector should walk the entire facility paying attention to where stormwater enters/exits and any eroded or poorly vegetated areas. Take pictures at strategic locations to supplement inspection notes.
- D. Documentation of the inspections should be completed including:
 - 1. Inspector's name, affiliation and professional credentials.
 - 2. Date, time and weather conditions.
 - 3. Rainfall total over the previous 24-hour period.
 - 4. Erosion and/or sediment accumulation.
 - 5. Vegetation condition of inlet and outlet conveyances including emergency spillway.
 - 6. Identification of potential structural failures and repair needs.
Repair recommendations.
- E. The inspection shall be filed with the regional Maintenance Department, the regional Stormwater & Erosion Control Engineer and the Statewide Stormwater Quality Engineer.

II. ROUTINE MAINTENANCE

- A. Debris and Litter Removal.
 - 1. Complete debris and litter removal from the infiltration trench surface during or following each inspection.

III. NON-ROUTINE MAINTENANCE

- A. Structural Repairs and/or Replacement

1. If any structural problems are noted in or around the infiltration trench, the inspector should initiate repairs and/or replacement immediately.
- B. Restoration of Infiltrative Capacity
1. Over time the original infiltrative capacity of the infiltration trench is expected to diminish.
 2. The inspections shall include an evaluation of drawdown time. If water is ponding more than 72 hours after rainfall stops entering the infiltration trench, the infiltrative capacity needs to be restored. The inspector shall initiate restoration of the infiltration basin soil profile per the original plans unless otherwise approved.
- D. Full Removal and Replacement of Infiltration Trench Profile
1. If the inspector determines that partial cleaning of the infiltration trench surface would not be sufficient to maintain infiltration capacity, full removal and replacement of the existing trench profile to permeable soils should be initiated to restore the infiltrative capacity. The replacement materials shall match the original design unless otherwise approved.
 2. Disturbed areas should be immediately re-vegetated and stabilized to prevent erosion and sedimentation.

WisDOT Stormwater Control Practice (SCP) Inspection Report Infiltration Trench

Part A: Site Information

Region	County	Highway
SCP ID:		SCP Description:
State Project No.:		Weather:
Inspector:		Inspection Date & Time:
Inspector Firm/Region:		Inspector's email:
24-hour Rainfall Total:		Last Rainfall End Date & Time:
SCP Construction Year:		Date of Last Inspection:
SCP Location:		

Part B: Maintenance Requirements

Summary of Urgent Maintenance Required: _____

Summary of Required Maintenance:

Assessment	Code	Location/Notes

Code Key			
N/A = Not Applicable	M = Monitor (potential for future problem)	NP = Not a Problem	WN = Work Needed

Part C: Detailed Inspection Notes

INFLOW POINTS

Assessment	Code	Comments
Obstruction: vegetation/debris/sediment		
Erosion/undercutting		
Displacement of fabric/rip rap		
Pipe Condition		
Other (describe)		

MAIN BASIN AREA

Assessment	Code	Comments
Standing water (> 3 days after storm) + % cover		
Sediment accumulation		
Vegetation height/type		
Bare soil/erosion		
Invasive vegetation (estimate a %)		
Weeds (estimate a %)		
Infiltration trench		

Cell dividers/level spreaders		
Other (describe)		

EMBANKMENT

Assessment	Code	Comments
Erosion and/or loss of dam material		
Shrubs/trees present		
Animal burrows		
Soft spots or settlement		
Emergency spillway		
Other (describe)		

OUTLET DEVICE

Assessment	Code	Comments
Obstruction: vegetation/debris/sediment		
Erosion/undercutting		
Joint failure/loss of joint material		
Outfall riprap/scour prevention		
Other (describe)		

MISCELLANEOUS

Assessment	Code	Comments
Trash/debris		
Access		
Vandalism		
Fence condition (if applicable)		
Wildlife observations		
Signage (if applicable)		
Sediment disposal site (available?)		
Other (describe)		

ADDITIONAL COMMENTS:

1. N/A

MAINTENANCE/REPAIR RECOMMENDATIONS:

1. N/A

INSTRUCTIONS FOR SCP INSPECTION FORM:

1. Complete Part A: Site Information
2. Complete Part C: Detailed Inspection Notes
3. Complete Part B: Maintenance Requirements
4. Attach color digital photographs of the site and structural SCPs using a photo log.
5. Update inspection spreadsheet

VEGETATED FILTER STRIPS INSPECTION AND MAINTENANCE GUIDELINES

I. INSPECTION

- A. Inspections of the filter strips should be completed every 5 years. Also inspect filter strips after a 5-inch rainfall in 24-hours to detect any potential structural failure problems.
- B. Inspect for erosion, accumulation of sediment, diversions and short circuiting, standing water and the accumulation of litter.

II. ROUTINE MAINTENANCE

- A. Mow when needed to maintain the vegetation at a height of six to twelve inches and to reduce woody vegetation.
- B. Minimize the use of fertilizers and pesticides.
- C. Do not use heavy equipment to mow. Mowing should occur during dry periods to avoid compaction.
- A. Repair and reseed eroded areas immediately upon discovery.

WisDOT Stormwater Control Practice (SCP) Inspection Report Vegetated Filter Strip

Part A: Site Information

Region:	County:	Highway:
SCP ID:	SCP Description:	
State Project No.:	Weather:	
Inspector:	Inspection Date & Time:	
Inspector Firm/Region:	Inspector's email:	
24-hour Rainfall Total:	Last Rainfall End Date & Time:	
SCP Construction Year:	Date of Last Inspection:	
SCP Location:		

Part B: Maintenance Requirements

Summary of Urgent Maintenance Required: _____

Summary of Required Maintenance:

Assessment	Code	Location/Notes

Code Key

N/A = Not Applicable	M = Monitor (potential for future problem)	NP = Not a Problem	WN = Work Needed
----------------------	--	--------------------	------------------

Part C: Detailed Inspection Notes

Assessment	Code	Comments
Erosion/Rilling		
Sediment accumulation		
Standing water		
Vegetation		
Visible pollution/litter		
Other (describe)		

ADDITIONAL COMMENTS:

1. N/A

MAINTENANCE/REPAIR RECOMMENDATIONS:

1. N/A

INSTRUCTIONS FOR SCP INSPECTION FORM:

1. Complete Part A: Site Information
2. Complete Part C: Detailed Inspection Notes
3. Complete Part B: Maintenance Requirements
4. Attach color digital photographs of the site and structural SCPs using a photo log.
5. Update inspection spreadsheet

**GRASS SWALES
INSPECTION AND MAINTENANCE GUIDELINES**

I. INSPECTION

- A. Inspections of the grass swales should be completed every 5 years. It is recommended to inspect swales after very large runoff events, such as after a 4-inch rainfall in 24-hours.
- B. Inspect for sediment deposition, erosion and litter.
- C. Nuisance conditions such as woody plant growth and mosquito breeding areas should also be identified and removed.

II. ROUTINE MAINTENANCE

- A. Mow when needed to maintain the vegetation at a height of six to twelve inches and to reduce woody vegetation.

III. NON-ROUTINE MAINTENANCE

- A. Deep Tilling/Subsoiling
 - 1. If the surface of the grass swale is showing signs of poor drainage or standing water, deep tilling or subsoiling may be needed to maintain infiltrative capacity.
 - 2. Deep tilled and subsoiled areas should be immediately re-vegetated to prevent erosion.

WisDOT Stormwater Control Practice (SCP) Inspection Report Enhanced Grass Swale

Part A: Site Information

Region:	County:	Highway:
SCP ID:	SCP Description:	
State Project No.:	Weather:	
Inspector:	Inspection Date & Time:	
Inspector Firm/Region:	Inspector's email:	
24-hour Rainfall Total:	Last Rainfall End Date & Time:	
SCP Construction Year:	Date of Last Inspection:	
SCP Location:		

Part B: Maintenance Requirements

Summary of Urgent Maintenance Required: _____

Summary of Required Maintenance:

Assessment	Code	Location/Notes

Code Key			
N/A = Not Applicable	M = Monitor (potential for future problem)	NP = Not a Problem	WN = Work Needed

Part C: Detailed Inspection Notes

Assessment	Code	Comments
Erosion		
Sediment accumulation		
Standing water		
Embankment Rilling		
Pipe Condition		
Visible pollution/litter		
Other (describe)		

ADDITIONAL COMMENTS:

1. N/A

MAINTENANCE/REPAIR RECOMMENDATIONS:

1. N/A

INSTRUCTIONS FOR SCP INSPECTION FORM:

1. Complete Part A: Site Information
2. Complete Part C: Detailed Inspection Notes
3. Complete Part B: Maintenance Requirements
4. Attach color digital photographs of the site and structural SCPs using a photo log.
5. Update inspection spreadsheet

WisDOT Stormwater Control Practices (SCP) Inspection Report Grass Swale

Part A: Site Information

Region	County	Highway
SCP ID:		SCP Description:
State Project No.:		Weather:
Inspector:		Inspection Date & Time:
Inspector Firm/Region:		Inspector's email:
24-hour Rainfall Total:		Last Rainfall End Date & Time:
SCP Construction Year:		Date of Last Inspection:
SCP Location:		

Part B: Maintenance Requirements

Summary of Urgent Maintenance Required: _____

Summary of Required Maintenance:

Assessment	Code	Location/Notes

Code Key			
N/A = Not Applicable	M = Monitor (potential for future problem)	NP = Not a Problem	WN = Work Needed

Part C: Detailed Inspection Notes

Assessment	Code	Comments
Erosion		
Sediment accumulation		
Standing water		
Visible pollution/litter		
Other (describe)		

ADDITIONAL COMMENTS:

1. N/A

MAINTENANCE/REPAIR RECOMMENDATIONS:

1. N/A

INSTRUCTIONS FOR SCP INSPECTION FORM:

1. Complete Part A: Site Information
2. Complete Part C: Detailed Inspection Notes
3. Complete Part B: Maintenance Requirements
4. Attach color digital photographs of the site and structural SCPs using a photo log.
5. Update inspection spreadsheet

Section G – Pollutant Loading Analysis Detail by County

Table G 1 – Runoff Volume and Pollutant Percent Reduction Summary by Region

Northeast Region						
County Name	Year Modeled	Miles Mapped	Total Highway Source Area (ac)	Percent Reduction		
				Runoff Volume (%)	Particulate Solids Yield (%)	Total Phosphorus (%)
Brown	2011	83.0	2,921	41%	51%	45%
	2013/2014	7.3	293	34%	43%	41%
	2020	14.6	551	14%	24%	18%
Calumet	2020	2.4	39	5%	11%	11%
Dodge	2013/2014	2.4	83	52%	63%	52%
Fond du Lac	2011	16.8	733	47%	56%	50%
	2013/2014	0.3	8	17%	33%	22%
	2020	14.2	232	14%	24%	17%
Marinette	2020	1.5	23	25%	32%	28%
Manitowoc	2011	9.0	207	47%	44%	41%
	2013/2014	1.6	37	10%	27%	18%
Outagamie	2011	43.5	1,384	26%	39%	34%
	2013/2014	10.6	305	21%	32%	26%
	2020	15.1	357	13%	22%	16%
Sheboygan	2011	21.8	718	31%	39%	36%
	2020	6.5	104	18%	29%	24%
Winnebago	2011	24.6	660	37%	48%	43%
	2013/2014	18.8	649	25%	35%	27%
	2020	19.0	775	5%	16%	11%
NE Region Total		312.9	10,078	24%	35%	29%

North Central Region						
County Name	Year Modeled	Miles Mapped	Total Highway Source Area (ac)	Percent Reduction		
				Runoff Volume (%)	Particulate Solids Yield (%)	Total Phosphorus (%)
Marathon	2011	36.8	1,496	43%	53%	45%
	2020	3.5	151	29%	36%	30%
Wood	2011	4.2	80	9%	13%	11%
	2020	0.4	10	8%	20%	14%
Lincoln	2011	3.0	44	9%	7%	9%
Portage	2011	10.7	317	69%	70%	62%
NC Region Total		58.7	2,098	41%	49%	41%

Northwest Region						
County Name	Year Modeled	Miles Mapped	Total Highway Source Area (ac)	Percent Reduction		
				Runoff Volume (%)	Particulate Solids Yield (%)	Total Phosphorus (%)
Barron	2020	3.6	160	14%	24%	17%
Chippewa	2013/2014	23.7	1028	69%	74%	71%
	2020	10.5	588	72%	76%	76%
Douglas	2011	12.0	328	21%	23%	27%
	2020	0.4	8	11%	16%	10%
Dunn	2011	8.3	320	51%	55%	49%
Eau Claire	2013/2014	38.2	1583	61%	67%	66%
	2020	15.8	604	53%	62%	65%
Pierce	2011	3.6	176	34%	30%	28%
	2020	1.5	32	63%	67%	63%
St. Croix	2020	5.7	266	35%	41%	39%
NW Region Total		122.9	5092	57%	64%	63%

Southeast Region						
County Name	Year Modeled	Miles Mapped	Total Highway Source Area (ac)	Percent Reduction		
				Runoff Volume (%)	Particulate Solids Yield (%)	Total Phosphorus (%)
Kenosha	2013/2014	45.1	1,161	14%	21%	17%
	2020	4.8	163	6%	13%	10%
Milwaukee	2013/2014	160.2	4,843	8%	13%	11%
	2020	17.5	528	0%	1%	1%
Ozaukee	2013/2014	27.8	676	31%	41%	38%
	2020	4.3	139	11%	19%	16%
Racine	2013/2014	23.9	399	5%	10%	9%
	2020	13.9	297	19%	27%	21%
Walworth	2020	12.1	502	29%	29%	21%
Washington	2013/2014	22.5	701	38%	48%	41%
	2020	32.5	687	14%	23%	18%
Waukesha	2013/2014	145.3	4,029	29%	39%	34%
	2020	23.1	582	7%	14%	11%
SE Region Total		533.0	14,704	17%	25%	22%

Southwest Region						
County Name	Year Modeled	Miles Mapped	Total Highway Source Area (ac)	Percent Reduction		
				Runoff Volume (%)	Particulate Solids Yield (%)	Total Phosphorus (%)
Columbia	2020	7.6	185	40%	47%	43%
Dane	2013/2014	79.7	3,092	29%	33%	30%
	2020	23.2	786	17%	26%	19%
	2021	2.1	151	-	66%	-
Dodge	2013/2014	6.0	190	38%	46%	42%
	2020	7.5	227	19%	28%	22%
Grant	2020	1.8	25	7%	20%	14%
Green	2013/2014	2.3	108	34%	42%	35%
	2020	1.1	42	15%	26%	18%
Jefferson	2013/2014	0.7	9	3%	5%	5%
	2020	6.7	161	13%	24%	16%
LaCrosse	2013/2014	29.5	1,207	45%	50%	46%
	2020	15.5	415	44%	49%	42%
Rock	2013/2014	26.6	838	53%	42%	51%
	2020	6.6	177	19%	26%	23%
	2021	8.9	4,086	-	62%	-
Sauk	2013/2014	0.8	9	0%	0%	0%
	2020	2.5	56	26%	33%	26%
SW Region Total		215.5	10,932	33%	41%	34%

Southwest Region Non-TS4 Pollutant Loading Reductions in the Rock River TMDL Basin						
County Name	Year Modeled	Miles Mapped	Total Highway Source Area (ac)	Percent Reduction		
				Runoff Volume (%)	Particulate Solids Yield (%)	Total Phosphorus (%)
Dane	2020	49.3	1,222	-	57%	41%
Rock	2020	18.2	1,115	-	67%	76%

Table G 2 – Runoff Volume and Pollutant Percent Reduction Summary by County

				Northeast Region															
				Base Condition					Grass Swale					Percent Reduction					
County Name	Year Modeled	Miles Mapped	Total Highway Source Area (ac)	Runoff Volume (cf)	Particulate Solids Yield (lbs)	Particulate Phosphorus (lbs)	Filterable Phosphorus (lbs)	Total Phosphorus (lbs)	Runoff Volume (cf)	Particulate Solids Yield (lbs)	Particulate Phosphorus (lbs)	Filterable Phosphorus (lbs)	Total Phosphorus (lbs)	Runoff Volume (%)	Particulate Solids Yield (%)	Particulate Phosphorus (%)	Filterable Phosphorus (%)	Total Phosphorus (%)	
Brown	2011	83.0	2,921	39,047,730	400,450	1,197	588	1,785	23,231,005	195,756	613	369	982	41%	51%	49%	37%	45%	
	2013/2014	7.3	293	8,674,143	105,714	302	163	490	5,695,305	60,564	188	114	291	34%	34%	43%	38%	41%	
	2020	15.0	551	24,550,000	252,945	791	454	1,245	21,130,000	191,692	614	405	1,019	14%	24%	22%	11%	18%	
Calumet	2020	2.0	39	2,572,000	23,503	48	25	73	2,441,000	20,828	42	24	65	5%	11%	13%	6%	11%	
Dodge	2013/2014	2.4	83	2,564,284	32,362	87	46	139	1,238,995	12,008	39	26	67	52%	63%	54%	43%	52%	
Fond du Lac	2011	16.8	733	9,268,775	86,042	284	156	440	4,896,041	38,036	131	88	219	47%	56%	54%	44%	50%	
	2013/2014	0.3	8	265,691	3,338	9	5	14	221,824	2,230	6	4	11	17%	33%	27%	11%	22%	
	2020	14.0	232	11,370,000	124,952	335	182	517	9,814,000	94,598	262	166	428	14%	24%	22%	9%	17%	
Marinette	2020	2.0	23	1,238,000	11,664	31	17	48	932,957	7,931	22	13	35	25%	32%	30%	24%	28%	
Manitowoc	2011	9.0	207	4,130,260	27,402	79	47	126	2,172,602	15,301	46	29	75	47%	44%	42%	39%	41%	
	2013/2014	1.6	37	1,949,515	16,682	39	19	62	1,209,605	12,237	30	18	51	10%	27%	23%	7%	18%	
	2020	43.5	1,384	25,674,636	320,135	842	326	1,169	18,893,194	195,569	528	248	776	26%	39%	37%	24%	34%	
Outagamie	2011	10.6	305	10,690,000	116,060	300	161	505	8,488,000	78,434	209	129	374	21%	32%	30%	20%	26%	
	2013/2014	10.6	305	17,310,000	172,980	498	282	781	15,070,000	134,159	397	257	654	13%	22%	20%	9%	16%	
	2020	15.0	357	17,310,000	172,980	498	282	781	15,070,000	134,159	397	257	654	13%	22%	20%	9%	16%	
Sheboygan	2011	21.8	718	11,160,486	98,005	293	155	449	7,706,749	59,895	179	99	287	31%	39%	31%	21%	26%	
	2020	6.0	104	5,756,000	57,810	141	76	216	4,700,000	41,086	101	63	165	18%	29%	28%	16%	24%	
	2011	24.6	660	12,135,353	130,321	356	156	512	7,697,137	68,292	192	102	293	37%	48%	46%	35%	43%	
Winnebago	2013/2014	18.8	649	22,227,322	238,658	636	346	1,076	16,734,246	155,589	451	281	787	25%	35%	29%	19%	27%	
	2020	19.0	775	34,930,000	346,657	1,096	637	1,733	33,150,000	289,898	928	617	1,545	5%	16%	15%	3%	11%	
	NE Total		312.5	10,078	244,914,195	2,565,678	7,361	3,843	11,379	185,422,660	1,674,103	4,978	3,061	8,124	24%	35%	32%	20%	29%

				North Central Region															
				Base Condition					Grass Swale					Percent Reduction					
Region Name	Year Modeled	Miles Mapped	Total Highway Source Area (ac)	Runoff Volume (cf)	Particulate Solids Yield (lbs)	Particulate Phosphorus (lbs)	Filterable Phosphorus (lbs)	Total Phosphorus (lbs)	Runoff Volume (cf)	Particulate Solids Yield (lbs)	Particulate Phosphorus (lbs)	Filterable Phosphorus (lbs)	Total Phosphorus (lbs)	Runoff Volume (%)	Particulate Solids Yield (%)	Particulate Phosphorus (%)	Filterable Phosphorus (%)	Total Phosphorus (%)	
Wausau	2011	36.8	174	23,274,407	185,895	536	289	825	13,167,209	87,037	272	181	453	43%	53%	49%	37%	45%	
Wood	2020	0.4	10	531,027	5,794	15	8	22	489,579	4,632	12	7	19	8%	20%	19%	5%	14%	
Marathon	2020	3.5	151	6,070,000	68,777	228	131	359	4,300,000	44,092	152	99	251	29%	36%	33%	24%	30%	
Merrill	2011	3.0	7	1,483,819	8,114	21	14	34	1,350,426	7,507	19	12	31	9%	7%	9%	10%	9%	
Marshfield	2011	3.8	9	2,708,748	9,623	27	25	52	2,605,248	8,936	25	24	49	4%	7%	8%	4%	6%	
Wisconsin Rapids	2011	0.4	1	204,642	834	2	2	3	42,720	131	0	0	1	79%	84%	83%	78%	81%	
Stevens Point	2011	10.7	39	5,569,798	46,383	126	63	189	1,744,960	13,760	44	28	72	69%	70%	65%	56%	62%	
NC Total		54.7	392	39,842,440	325,420	954	531	1,485	23,700,143	166,094	524	352	876	41%	49%	45%	34%	41%	

				Northwest Region															
				Base Condition					Grass Swale					Percent Reduction					
County Name	Year Modeled	Miles Mapped	Total Highway Source Area (ac)	Runoff Volume (cf)	Particulate Solids Yield (lbs)	Particulate Phosphorus (lbs)	Filterable Phosphorus (lbs)	Total Phosphorus (lbs)	Runoff Volume (cf)	Particulate Solids Yield (lbs)	Particulate Phosphorus (lbs)	Filterable Phosphorus (lbs)	Total Phosphorus (lbs)	Runoff Volume (%)	Particulate Solids Yield (%)	Particulate Phosphorus (%)	Filterable Phosphorus (%)	Total Phosphorus (%)	
Barron	2020	3.6	160	6,055,000	73,672	250	142	392	5,183,000	55,980	196	128	324	14%	24%	22%	10%	17%	
Chippewa	2013/2014	23.7	1,028	29,110,000	370,134	1,127	618	1,811	8,978,000	96,001	319	203	528	69%	74%	72%	67%	71%	
	2020	10.0	588	21,140,000	257,842	921	531	1,452	5,911,000	62,280	216	136	352	72%	76%	77%	74%	76%	
Douglas	2011	12.0	328	6,359,966	49,988	146	80	226	5,040,292	38,593	106	60	166	21%	23%	27%	25%	27%	
	2020	0.4	8	347,614	3,719	11	6	18	308,209	3,119	10	6	16	11%	16%	12%	6%	10%	
Dunn	2011	8.3	320	4,229,806	36,538	104	53	157	2,064,185	16,317	51	30	81	51%	51%	51%	44%	49%	
Eau Claire	2013/2014	38.2	1,583	47,774,000	594,416	1,723	934	2,782	18,715,503	198,494	596	369	947	61%	67%	65%	61%	66%	
	2020	15.8	604	26,760,000	283,247	876	499	1,375	12,600,000	106,620	294	183	477	53%	62%	66%	63%	65%	
Pierce	2011	3.6	176	1,740,653	17,063	52	26	78	1,140,409	11,983	37	19	56	34%	30%	28%	27%	28%	
	2020	1.5	32	1,280,000	15,159	49	27	76	473,808	5,049	17	11	29	63%	67%	65%	59%	63%	
St. Croix	2020	5.7	266	10,900,000	118,065	395	228	623	7,131,000	69,488	234	147	381	35%	41%	41%	36%	39%	
NW Total		122.9	5,092	155,697,039	1,819,844	5,651	3,145	8,990	67,545,405	663,924	2,076	1,291	3,356	57%	64%	63%	59%	63%	

				Southeast Region															
				Base Condition					Grass Swale					Percent Reduction					
County Name	Year Modeled	Miles Mapped	Total Highway Source Area (ac)	Runoff Volume (cf)	Particulate Solids Yield (lbs)	Particulate Phosphorus (lbs)	Filterable Phosphorus (lbs)	Total Phosphorus (lbs)	Runoff Volume (cf)	Particulate Solids Yield (lbs)	Particulate Phosphorus (lbs)	Filterable Phosphorus (lbs)	Total Phosphorus (lbs)	Runoff Volume (%)	Particulate Solids Yield (%)	Particulate Phosphorus (%)	Filterable Phosphorus (%)	Total Phosphorus (%)	
Kenosha	2013/2014	45.1	1,161	57,230,837	598,753	1,719	963	2,682	49,419,440	471,242	1,387	899	2,226	14%	21%	19%	13%	17%	
	2020	4.8	163	8,638,000	73,432	207	122	329	8,139,000	64,019	183	115	298	6%	13%	12%	6%	10%	
Milwaukee	2013/2014	160.2	4,843	241,018,897	2,478,772	7,104	3,995	11,099	221,995,158	2,155,204	6,238	3,687	9,924	8%	13%	12%	8%	11%	
	2020	17.5	528	30,290,000	240,691	635	375	1,010	30,150,000	238,166	629	373	1,002	0%	1%	1%	0%	1%	
Ozaukee	2013/2014	27.8	676	33,497,000	359,715	1,009	559	1,824	22,984,000	210,801	590	363	1,131	31%	41%	42%	35%	38%	
	2020	4.3	139	6,187,000	66,943	203	115	318	5,536,000	54,099	165	104	268	11%	19%	19%	9%	16%	
Racine	2013/2014	23.9	399	26,103,000	238,214	512	273	785	24,768,000	213,894	459	257	717	5%	10%	10%	6%	9%	
	2020	14.0	297	13,930,000	155,707	435	238	674	11,340,000	113,323	329	205	534	19%	27%	24%	14%	21%	
Walworth	2013/2014	22.5	701	29,757,000	345,949	1,106	624	1,730	18,357,409	178,298	614	399	1,013	38%	48%	44%	36%	41%	
	2020	32.5	687	33,150,000	350,528	980	544	1,524	28,420,000	270,019	769	481	1,250	14%	23%	22%	12%	18%	
Waukesha	2013/2014	145.3	4,029	196,855,664	2,072,323	5,985	3,356	10,878	139,924,852	1,268,183	3,782	2,378	7,144	29%	37%	37%	29%	34%	
	2020	23.1	582	23,400,000	282,415	791	449	1,240	27,350,000	243,533	680	419	1,098	7%	14%	14%	7%	11%	
SE Total		533.0	14,704	724,907,398	7,493,606	21,470	12,058	35,321	603,543,859	5,645,257	16,409	10,002	27,572	17%	25%	24%	17%	22%	

				Southwest Region															
				Base Condition					Grass Swale					Percent Reduction					
County Name	Year Modeled	Miles Mapped	Total Highway Source Area (ac)	Runoff Volume (cf)	Particulate Solids Yield (lbs)	Particulate Phosphorus (lbs)	Filterable Phosphorus (lbs)	Total Phosphorus (lbs)	Runoff Volume (cf)	Particulate Solids Yield (lbs)	Particulate Phosphorus (lbs)	Filterable Phosphorus (lbs)	Total Phosphorus (lbs)	Runoff Volume (%)	Particulate Solids Yield (%)	Particulate Phosphorus (%)	Filterable Phosphorus (%)	Total Phosphorus (%)	
Columbia	2020	7.6	185	8,236,000	93,570	275	153	428	4,922,000	49,407	149	95	244	40%	47%	46%	38%	43%	
	2013/2014	77.1	2,846	128,457,786	1,345,381	4,244	2,406	6,650	84,674,082	862,545	2,825	1,793	4,617	34%	36%	33%	26%	31%	
Dane	2020	22.6	762	34,100,000	362,875	1,106	628	1,734	28,390,000	267,178	846	553	1,399	17%	26%	23%	12%	19%	
	2021	2.1	131	-	84,680	-	-	400	-	28,900	-	-	143	-	66%	-	-	64%	
	2013/2014	6.0	190	7,900,370	92,820	290	162	452	4,862,121	50,108	160	104	264	38%	46%	45%	36%	42%	
Dodge	2020	7.5	227	9,926,000	112,187	337	189	526	8,041,000	80,569	249	160	409	19%	28%	26%	15%	22%	
	2013/2014	1.8	25	1,262,000	13,963	36	19	55	1,173,000	11,145	29	18	48	7%	20%	19%	5%	14%	
Grant	2013/2014	2.3	108	4,134,747	50,116	167	95	262	2,715,327	28,957	103	68	171	34%	42%	38%	28%	35%	
	2020	1.1	42	1,744,000	20,627	65	36	101	1,478,000	15,350	50	32	82	15%	23%	23%	10%	18%	
Green	2013/2014	0.7	9	686,560	5,740	9	4	13	668,297	5,467	8	4	13	3%	5%	6%	3%	5%	
	2020	6.7	161	6,763,000	79,967	244	136	380	5,884,000	61,045	194	126	319	13%	24%	21%	8%	16%	
Jefferson	2013/2014	29.5	1,207	46,731,972	544,212	1,847	1,059	2,907	25,798,231	270,074	958	612	1,570	45%	50%	48%	42%	46%	
	2020	15.5	415	18,610,000	214,505	620	341	961	10,420,000	108,893	340	217	557	44%	49%	45%	36%	42%	
LaCrosse	2013/2014	17.7	361	16,631,264	189,690	536	294	830	7,542,682	78,697	234	145	378	55%	59%	56%	51%	54%	
	2020	4.9	82	4,091,000	39,490	112	64	176	3,550,000	32,729	93	55	149	17%	17%	17%	14%	16%	
	2021	6.5	766	-	502,340	-	-	2,184	-	193,180	-	-	977	-	62%	-	-	55%	
Sauk	2013/2014	0.8	9	758,924	6,259	9	4	14	758,924	6,259	9	4	14	0%	0%	0%	0%	0%	
	2020	2.5	56	2,473,000	27,591	82	46	128	1,839,000	18,390	58	37	95	26%	33%	30%	20%	26%	
SW Total		213.1	7,581	292,506,623	3,786,013	9,981	5,636	18,202	192,716,664	2,168,893	6,304	4,025	11,449	34%	43%	37%	29%	37%	

				Statewide															
				Base Condition					Grass Swale					Percent Reduction					
County Name	Year Modeled	Miles Mapped	Total Highway Source Area (ac)	Runoff Volume (cf)	Particulate Solids Yield (lbs)	Particulate Phosphorus (lbs)	Filterable Phosphorus (lbs)	Total Phosphorus (lbs)	Runoff Volume (cf)	Particulate Solids Yield (lbs)	Particulate Phosphorus (lbs)	Filterable Phosphorus (lbs)	Total Phosphorus (lbs)	Runoff Volume (%)	Particulate Solids Yield (%)	Particulate Phosphorus (%)	Filterable Phosphorus (%)	Total Phosphorus (%)	
NE Total																			
NC Total		312.5	10,078	244,914,195	2,565,678	7,361	3,843	11,379	185,422,660	1,674,103	4,978	3,061	8,124	24%	35%	32%	20%	29%	
NW Total		54.7	392	39,842,440	325,420	954	531	1,485	23,700,143	166,094	524	352	876	41%	49%	45%	34%	41%	
SE Total		122.9	5,092	155,697,039	1,819,844	5,651	3,145	8,990	67,545,405	663,924	2,076	1,291	3,356	57%	64%	63%	59%	63%	
SW Total		533.0	14,704	724,907,398	7,493,606	21,470	12,058	35,321	603,543,859	5,645,257	16,409	10,002	27,572	17%	25%	24%	17%	22%	
SE Total		213.1	7,581	292,506,623	3,786,013	9,981	5,636	18,202	192,716,664	2,168,893	6,304	4,025	11,449	34%	43%	37%	29%	37%	
SE Total		1,236.2	37,847	1,457,867,695	15,990,561	45,418	25,214	75,377	1,072,928,731	10,318,271	30,291	18,730	51,375	26%	35%	33%	26%	32%	

Section H – WisDOT Statewide Stormwater Control Practice Database

SCP ID	SCP TY	Final SCP Name	PROJ_ID	PROJ_DESC	SCP_DESC	CNTY	DOT_RGN	ROAD_SYS	ROAD_N M	Maintenance Responsibility	Permit Cycle 1 (2018-2023) Inspection Date	Permit Cycle 2 (2023-2028) Inspection Date	Next Planned SCP Insp Date	PC1 Inspection Duration (hrs)	PC2 Inspection Duration (hrs)	Urgent Maint- enance	Inspection Notes / Maintenance-Repair Recommendation
1	Biofilter	Biofilter	49840114	College Avenue, City of Appleton Fox River Bridge and Approaches	West Biofilter	Outagamie	Northeast	Local Road	College Avenue	Local			5/1/2025				
2	Biofilter	Biofilter	49840114	College Avenue, City of Appleton Fox River Bridge and Approaches	East Biofilter	Outagamie	Northeast	Local Road	College Avenue	Local			5/1/2025				
3	Biofilter	Biofilter	46591371	College Avenue, City of Appleton Drew St. - S. Alton Ct.	Banta Biofilter	Outagamie	Northeast	Local Road	College Avenue	Local			5/1/2025				
4	Biofiltration basin	Biofilter	13001370	Hoan Bridge & Lake Freeway Milwaukee River to Carferry Dr Bridge Rehabilitation	BF-A	Milwaukee	Southeast	IH	794	State	7/20/2023		5/1/2028	0.25		0	
5	Biofiltration basin	Biofilter	13001370	Hoan Bridge & Lake Freeway Milwaukee River to Carferry Dr Bridge Rehabilitation	BF-B	Milwaukee	Southeast	IH	794	State	7/20/2023		5/1/2028	0.25		0	
6	Biofiltration basin	Biofilter	13001370	Hoan Bridge & Lake Freeway Milwaukee River to Carferry Dr Bridge Rehabilitation	BF-C	Milwaukee	Southeast	IH	794	State	7/20/2023		5/1/2028	0.25		0	
7	Biofiltration basin	Biofilter	13001370	Hoan Bridge & Lake Freeway Milwaukee River to Carferry Dr Bridge Rehabilitation	BF-D	Milwaukee	Southeast	IH	794	State	7/20/2023		5/1/2028	0.25		0	
8	Bioretention basin	Biofilter	31900070	Lake Geneva to East Troy Road USH 12 to N Side of Como Creek Brdg	Bioretention Basin A	Walworth	Southeast	STH	120	State		6/4/2024	5/1/2029		0.30	0	2" sediment at IP1 to be removed. Monitor bare spots on northwest embankment, monitor IP2 erosion.
9	Bioretention basin	Biofilter	31900070	Lake Geneva to East Troy Road USH 12 to N Side of Como Creek Brdg	Bioretention Basin B	Walworth	Southeast	STH	120	State		6/4/2024	6/4/2024		0.50	0	Monitor erosion at IP2 and outlet device.
10	Biofiltration device	Biofilter	20251270	East Capitol Drive Estabrook Parkway to Lake Drive	Biofiltration device 1	Milwaukee	Southeast	STH	190	Connecting Hwy			5/1/2025	0.00			
11	Biofiltration device	Biofilter	20251270	East Capitol Drive Estabrook Parkway to Lake Drive	Biofiltration device 2	Milwaukee	Southeast	STH	190	Connecting Hwy			5/1/2025	0.00			
12	Biofiltration device	Biofilter	20251270	East Capitol Drive Estabrook Parkway to Lake Drive	Biofiltration device 3	Milwaukee	Southeast	STH	190	Connecting Hwy			5/1/2025	0.00			
13	Biofiltration device	Biofilter	20251270	East Capitol Drive Estabrook Parkway to Lake Drive	Biofiltration device 4	Milwaukee	Southeast	STH	190	Connecting Hwy			5/1/2025	0.00			
14	Biofiltration device	Biofilter	20251270	East Capitol Drive Estabrook Parkway to Lake Drive	Biofiltration device 5	Milwaukee	Southeast	STH	190	Connecting Hwy			5/1/2025	0.00			
15	Biofiltration device	Biofilter	20251270	East Capitol Drive Estabrook Parkway to Lake Drive	Biofiltration device 6	Milwaukee	Southeast	STH	190	Connecting Hwy			5/1/2025	0.00			
16	Biofiltration device	Biofilter	20251270	East Capitol Drive Estabrook Parkway to Lake Drive	Biofiltration device 7	Milwaukee	Southeast	STH	190	Connecting Hwy			5/1/2025	0.00			
17	Biofiltration device	Biofilter	20251270	East Capitol Drive Estabrook Parkway to Lake Drive	Biofiltration device 8A	Milwaukee	Southeast	STH	190	Connecting Hwy			5/1/2025	0.00			
18	Biofiltration device	Biofilter	20251270	East Capitol Drive Estabrook Parkway to Lake Drive	Biofiltration device 8B	Milwaukee	Southeast	STH	190	Connecting Hwy			5/1/2025	0.00			
19	Biofiltration device	Biofilter	20251270	East Capitol Drive Estabrook Parkway to Lake Drive	Biofiltration device 9	Milwaukee	Southeast	STH	190	Connecting Hwy			5/1/2025	0.00			
20	Biofiltration device	Biofilter	20251270	East Capitol Drive Estabrook Parkway to Lake Drive	Biofiltration device 10	Milwaukee	Southeast	STH	190	Connecting Hwy			5/1/2025	0.00			
21	Biofiltration device	Biofilter	10302078	N-S Freeway - Airport Spur Airport Spur Mainline	Biofiltration Device 1	Milwaukee	Southeast	STH	119	State	7/13/2023		5/1/2028	0.30		0	Mostly trees in Main Basin
22	Biofiltration device	Biofilter	10302078	N-S Freeway - Airport Spur Airport Spur Mainline	Biofiltration device 2	Milwaukee	Southeast	STH	119	State	7/13/2023		5/1/2028	0.30		0	Trees growing on fence gate. Unable to open.
23	Biofiltration device	Biofilter	10302078	N-S Freeway - Airport Spur Airport Spur Mainline	Biofiltration Device 3	Milwaukee	Southeast	STH	119	State	7/13/2023		5/1/2028	0.25		0	
24	Biofiltration device	Biofilter	10302085	N-S Freeway - Ryan Road Interchange Ryan Rd Bridge & Ramps	Biofiltration device	Milwaukee	Southeast	IH	94	State	6/20/2023		5/1/2028	0.25		0	Located East of given coordinates.
25	Biofiltration basin	Biofilter	92690010	Village of Howard, Glendale Avenue, Evergreen Ave - Spring Green Road	Biofiltration basin 1	Brown	Northeast	Local Road	Glendale Avenue	Local			5/1/2025				
26	Bioretention Basin	Biofilter	12060778	Mount-Horeb-Madison Raymond Road to Madison Beltline	Bioretention Basin C	Dane	Southwest	USH	18	State	5/23/2022		5/1/2027			0	Dead trees around basin should be removed.
27	Bioretention basin	Biofilter	12060778	Mount-Horeb-Madison Raymond Road to Madison Beltline	Bioretention basin D	Dane	Southwest	USH	18	State	5/23/2022		5/1/2027			0	Additional riprap needed along channel.
28	Bioretention basin	Biofilter	12060774	Mount Horeb - Madison CTH PD to Raymond Rd Stg 2/Phase 1	Bioretention Basin B	Dane	Southwest	USH	18	State	6/15/2022		5/1/2027			0	Leaves clogging endwall #3 should be removed.
29	Bioretention Basin	Biofilter	12060774	Mount Horeb - Madison CTH PD to Raymond Rd Stg 2/Phase 1	Bioretention Basin C	Dane	Southwest	USH	18	State	6/15/2022		5/1/2027			0	
30	Bioretention Basin	Biofilter	89990062	City of Hudson, Wisconsin Street, 11th Street - Vine Street	Bioretention System	St. Croix	Northwest	Local Road	Wisconsin Street	Local							
31	Bioretention Area	Biofilter	23650670	W. Grange Ave. and S. 60th St, 76th - Loomis and 1500' North and South of W. Grange Ave.	Bio Area 1	Milwaukee	Southeast	Local Road	W. Grange Avenue	Local			5/1/2025				
32	Bioretention Area	Biofilter	23650670	W. Grange Ave. and S. 60th St, 76th - Loomis and 1500' North and South of W. Grange Ave.	Bio Area 2	Milwaukee	Southeast	Local Road	W. Grange Avenue	Local			5/1/2025				
33	Bioretention Area	Biofilter	23650670	W. Grange Ave. and S. 60th St, 76th - Loomis and 1500' North and South of W. Grange Ave.	Bio Area 3	Milwaukee	Southeast	Local Road	W. Grange Avenue	Local			5/1/2025				
34	Bioretention Area	Biofilter	23650670	W. Grange Ave. and S. 60th St, 76th - Loomis and 1500' North and South of W. Grange Ave.	Bio Area 4	Milwaukee	Southeast	Local Road	W. Grange Avenue	Local			5/1/2025				
35	Bioretention Basin	Biofilter	12060775	Mount Horeb-Madison CTH PD to Raymond Rd/Stg 2/Phase 2	Bioretention basin	Dane	Southwest	USH	18	State	6/15/2022	9/9/2024	5/1/2029	0.50	0.50	0	Monitor: Vegetation in riprap at IP2
36	Bioretention Basin	Biofilter	11201070	STH 26 - Breezewood Lane, STH 26 - STH 21, Salt Storage Facility	Bioretention Basin	Winnebago	Northeast	Off System	Salt Storage Facility	State		9/30/2024	5/1/2029		0.50	0	Work Needed: Remove invasive vegetation in main basin. Monitor: Bare soil in SE main basin, phragmites, bare soil on NE embankments
37	Bioretention Swale	Biofilter	13320070	Antioch Ill. To Paddock Lake Road, State Line to STH 50	Bioretention Swale	Kenosha	Southeast	STH	83	State	7/17/2023		5/1/2028	0.50		0	Monitor Endwall #4 (~75% clogged) for sediment accumulation.
38	Bioswale	Biofilter	22650870	27th Street City of Milwaukee, Howard Avenue to Union Pacific Railroad	Bio-swale No. 1	Milwaukee	Southeast	STH	241	Connecting Hwy	7/19/2023		7/19/2023	0.25		0	
39	Bioswale	Biofilter	22650870	27th Street City of Milwaukee, Howard Avenue to Union Pacific Railroad	Bio-swale No. 2	Milwaukee	Southeast	STH	241	Connecting Hwy	7/19/2023		7/19/2023	0.25		0	
40	Bioswale	Biofilter	22650870	27th Street City of Milwaukee, Howard Avenue to Union Pacific Railroad	Bio-swale No. 3	Milwaukee	Southeast	STH	241	Connecting Hwy	7/19/2023		7/19/2023	0.25		0	
41	Bioswale	Biofilter	22650870	27th Street City of Milwaukee, Howard Avenue to Union Pacific Railroad	Bio-swale No. 4	Milwaukee	Southeast	STH	241	Connecting Hwy	7/19/2023		7/19/2023	0.25		0	
42	Bioswale	Biofilter	22650870	27th Street City of Milwaukee, Howard Avenue to Union Pacific Railroad	Bio-swale No. 5	Milwaukee	Southeast	STH	241	Connecting Hwy	7/19/2023		7/19/2023	0.25		0	
43	Bioswale	Biofilter	22650870	27th Street City of Milwaukee, Howard Avenue to Union Pacific Railroad	Bio-swale No. 6	Milwaukee	Southeast	STH	241	Connecting Hwy	7/19/2023		7/19/2023	0.25		0	
44	Bioswale	Biofilter	22650870	27th Street City of Milwaukee, Howard Avenue to Union Pacific Railroad	Bio-swale No. 7	Milwaukee	Southeast	STH	241	Connecting Hwy	7/19/2023		7/19/2023	0.25		0	
45	Bioswale	Biofilter	22650870	27th Street City of Milwaukee, Howard Avenue to Union Pacific Railroad	Bio-swale No. 8	Milwaukee	Southeast	STH	241	Connecting Hwy	7/19/2023		7/19/2023	0.25		0	
46	Bioswale	Biofilter	22650870	27th Street City of Milwaukee, Howard Avenue to Union Pacific Railroad	Bio-swale No. 9	Milwaukee	Southeast	STH	241	Connecting Hwy	7/19/2023		7/19/2023	0.25		0	

SCP ID	SCP TY	Final SCP Name	PROJ ID	PROJ_DESC	SCP_DESC	CNTY	DOT_RGN	ROAD_SYS	ROAD_N M	Maintenance Responsibility	Permit Cycle 1 (2018-2023) Inspection Date	Permit Cycle 2 (2023-2028) Inspection Date	Next Planned SCP Insp Date	PC1 Inspection Duration (hrs)	PC2 Inspection Duration (hrs)	Urgent Maint- enance	Inspection Notes / Maintenance-Repair Recommendation
47	Bioswale	Biofilter	25450371	West Hampton Avenue, North Teutonia Avenue to North Green Bay Avenue	Bio-swale No. 1	Milwaukee	Southeast	Local Road	West Hampton Road	Local			5/1/2025				
48	Bioswale	Biofilter	25450371	West Hampton Avenue, North Teutonia Avenue to North Green Bay Avenue	Bio-swale No. 2	Milwaukee	Southeast	Local Road	West Hampton Road	Local			5/1/2025				
49	Bioswale	Biofilter	25450371	West Hampton Avenue, North Teutonia Avenue to North Green Bay Avenue	Bio-swale No. 3	Milwaukee	Southeast	Local Road	West Hampton Road	Local			5/1/2025				
50	Bioswale	Biofilter	25450371	West Hampton Avenue, North Teutonia Avenue to North Green Bay Avenue	Bio-swale No. 4	Milwaukee	Southeast	Local Road	West Hampton Road	Local			5/1/2025				
51	Bioswale	Biofilter	25450371	West Hampton Avenue, North Teutonia Avenue to North Green Bay Avenue	Bio-swale No. 5	Milwaukee	Southeast	Local Road	West Hampton Road	Local			5/1/2025				
52	Bioswale	Biofilter	25450371	West Hampton Avenue, North Teutonia Avenue to North Green Bay Avenue	Bio-swale No. 6	Milwaukee	Southeast	Local Road	West Hampton Road	Local			5/1/2025				
53	Bioswale	Biofilter	25450371	West Hampton Avenue, North Teutonia Avenue to North Green Bay Avenue	Bio-swale No. 7	Milwaukee	Southeast	Local Road	West Hampton Road	Local			5/1/2025				
54	Bioswale	Biofilter	20601170	Howell Avenue - City of Milwaukee, Layton Avenue to Howard Avenue	Bio-swale No. 1	Milwaukee	Southeast	STH	38	Connecting Hwy	7/13/2023		7/9/2023	0.25		0	Unable to locate. Likely removed.
55	Bioswale	Biofilter	20601170	Howell Avenue - City of Milwaukee, Layton Avenue to Howard Avenue	Bio-swale No. 2	Milwaukee	Southeast	STH	38	Connecting Hwy	7/13/2023		7/9/2023	0.25		0	
56	Bioswale	Biofilter	20601170	Howell Avenue - City of Milwaukee, Layton Avenue to Howard Avenue	Bio-swale No. 3	Milwaukee	Southeast	STH	38	Connecting Hwy	7/13/2023		7/9/2023	0.25		0	
57	Bioswale	Biofilter	20601170	Howell Avenue - City of Milwaukee, Layton Avenue to Howard Avenue	Bio-swale No. 4	Milwaukee	Southeast	STH	38	Connecting Hwy	7/13/2023		7/9/2023	0.25		0	
58	Bioswale	Biofilter	20601170	Howell Avenue - City of Milwaukee, Layton Avenue to Howard Avenue	Bio-swale No. 5	Milwaukee	Southeast	STH	38	Connecting Hwy	7/13/2023		7/9/2023	0.25		0	
59	Bioswale	Biofilter	20601170	Howell Avenue - City of Milwaukee, Layton Avenue to Howard Avenue	Bio-swale No. 6	Milwaukee	Southeast	STH	38	Connecting Hwy	7/13/2023		7/9/2023	0.25		0	
60	Bioswale	Biofilter	20701070	East Layton Avenue, South Howell Avenue to STH 794	Bio-swale No. 1	Milwaukee	Southeast	CTH	Y	Local			5/1/2025				
61	Bioswale	Biofilter	20701070	East Layton Avenue, South Howell Avenue to STH 794	Bio-swale No. 2	Milwaukee	Southeast	CTH	Y	Local			5/1/2025				
62	Bioswale	Biofilter	20701070	East Layton Avenue, South Howell Avenue to STH 794	Bio-swale No. 3	Milwaukee	Southeast	CTH	Y	Local			5/1/2025				
63	Bioswale	Biofilter	20701070	East Layton Avenue, South Howell Avenue to STH 794	Bio-swale No. 4	Milwaukee	Southeast	CTH	Y	Local			5/1/2025				
64	Bioswale	Biofilter	20701070	East Layton Avenue, South Howell Avenue to STH 794	Bio-swale No. 5	Milwaukee	Southeast	CTH	Y	Local			5/1/2025				
65	Bioswale	Biofilter	20701070	East Layton Avenue, South Howell Avenue to STH 794	Bio-swale No. 6	Milwaukee	Southeast	CTH	Y	Local			5/1/2025				
66	Infiltration basin	Infiltration Basin	10051071	Illinois State Line - Madison, Rock River Bridges B-53-0357/0358	Special Infiltration Area	Rock	Southwest	IH	39	Local			5/1/2025				
67	Retired																
68	Infiltration basin	Infiltration Basin	10051071	Illinois State Line - Madison, Rock River Bridges B-53-0357/0358	Special Infiltration Area	Rock	Southwest	IH	39	State		5/15/2024	5/1/2029		0.25	0	No Comments
69	Infiltration basin	Infiltration Basin	10051071	Illinois State Line - Madison, Rock River Bridges B-53-0357/0358	Special Infiltration Area	Rock	Southwest	IH	39	State	6/1/2021		5/1/2026				
70	Infiltration basin	Infiltration Basin	10051071	Illinois State Line - Madison, Rock River Bridges B-53-0357/0358	Special Infiltration Area	Rock	Southwest	IH	39	State	6/1/2021		5/1/2026				
71	Bioswale	Biofilter	22001370	West Highland Avenue, City of Milwaukee, North 27th Street to North 12th Street	Bio-swale No. 1	Milwaukee	Southeast	USH	18	Connecting Hwy						0.00	
72	Bioswale	Biofilter	22001370	West Highland Avenue, City of Milwaukee, North 27th Street to North 12th Street	Bio-swale No. 2	Milwaukee	Southeast	USH	18	Connecting Hwy						0.00	
73	Bioswale	Biofilter	22001370	West Highland Avenue, City of Milwaukee, North 27th Street to North 12th Street	Bio-swale No. 3	Milwaukee	Southeast	USH	18	Connecting Hwy						0.00	
74	Bioswale	Biofilter	22001370	West Highland Avenue, City of Milwaukee, North 27th Street to North 12th Street	Bio-swale No. 4	Milwaukee	Southeast	USH	18	Connecting Hwy						0.00	
75	Bioswale	Biofilter	22001370	West Highland Avenue, City of Milwaukee, North 27th Street to North 12th Street	Bio-swale No. 5	Milwaukee	Southeast	USH	18	Connecting Hwy						0.00	
76	Bioswale	Biofilter	22001370	West Highland Avenue, City of Milwaukee, North 27th Street to North 12th Street	Bio-swale No. 6	Milwaukee	Southeast	USH	18	Connecting Hwy						0.00	
77	Bioswale	Biofilter	22001370	West Highland Avenue, City of Milwaukee, North 27th Street to North 12th Street	Bio-swale No. 7	Milwaukee	Southeast	USH	18	Connecting Hwy						0.00	
78	Bioswale	Biofilter	22001370	West Highland Avenue, City of Milwaukee, North 27th Street to North 12th Street	Bio-swale No. 8	Milwaukee	Southeast	USH	18	Connecting Hwy						0.00	
79	Bioswale	Biofilter	22001370	West Highland Avenue, City of Milwaukee, North 27th Street to North 12th Street	Bio-swale No. 9	Milwaukee	Southeast	USH	18	Connecting Hwy						0.00	
80	Bioswale	Biofilter	22001370	West Highland Avenue, City of Milwaukee, North 27th Street to North 12th Street	Bio-swale No. 10	Milwaukee	Southeast	USH	18	Connecting Hwy						0.00	
81	Bioswale	Biofilter	22001370	West Highland Avenue, City of Milwaukee, North 27th Street to North 12th Street	Bio-swale No. 11	Milwaukee	Southeast	USH	18	Connecting Hwy						0.00	
82	Bioswale	Biofilter	22001370	West Highland Avenue, City of Milwaukee, North 27th Street to North 12th Street	Bio-swale No. 12	Milwaukee	Southeast	USH	18	Connecting Hwy						0.00	
83	Bioswale	Biofilter	22001370	West Highland Avenue, City of Milwaukee, North 27th Street to North 12th Street	Bio-swale No. 13	Milwaukee	Southeast	USH	18	Connecting Hwy						0.00	
84	Bioswale	Biofilter	22001370	West Highland Avenue, City of Milwaukee, North 27th Street to North 12th Street	Bio-swale No. 14	Milwaukee	Southeast	USH	18	Connecting Hwy						0.00	
85	Bioswale	Biofilter	22001370	West Highland Avenue, City of Milwaukee, North 27th Street to North 12th Street	Bio-swale No. 15	Milwaukee	Southeast	USH	18	Connecting Hwy						0.00	
86	Bioswale	Biofilter	25040071	North 91st Street, West Mill Road to West Good Hope Road	Bio-swale No. 1	Milwaukee	Southeast	Local Road	North 91st Street	Local							

SCP ID	SCP TY	Final SCP Name	PROJ_ID	PROJ_DESC	SCP_DESC	CNTY	DOT_RGN	ROAD_SYS	ROAD_N M	Maintenance Responsibility	Permit Cycle 1 (2018-2023) Inspection Date	Permit Cycle 2 (2023-2028) Inspection Date	Next Planned SCP Insp Date	PC1 Inspection Duration (hrs)	PC2 Inspection Duration (hrs)	Urgent Maint- enance	Inspection Notes / Maintenance-Repair Recommendation
87	Bioswale	Biofilter	25040071	North 91st Street, West Mill Road to West Good Hope Road	Bio-swale No. 2	Milwaukee	Southeast	Local Road	North 91st Street	Local							
88	Bioswale	Biofilter	25040071	North 91st Street, West Mill Road to West Good Hope Road	Bio-swale No. 3	Milwaukee	Southeast	Local Road	North 91st Street	Local							
89	Bioswale	Biofilter	25040071	North 91st Street, West Mill Road to West Good Hope Road	Bio-swale No. 4	Milwaukee	Southeast	Local Road	North 91st Street	Local							
90	Bioswale	Biofilter	25040071	North 91st Street, West Mill Road to West Good Hope Road	Bio-swale No. 5	Milwaukee	Southeast	Local Road	North 91st Street	Local							
91	Bioswale	Biofilter	25040071	North 91st Street, West Mill Road to West Good Hope Road	Bio-swale No. 6	Milwaukee	Southeast	Local Road	North 91st Street	Local							
92	Bioswale	Biofilter	25040071	North 91st Street, West Mill Road to West Good Hope Road	Bio-swale No. 7	Milwaukee	Southeast	Local Road	North 91st Street	Local							
93	Bioretention swale	Biofilter	29800070	W. Edgerton Avenue, W. Loomis Road to S. 27th Street	Drainage Depressional Area	Milwaukee	Southeast	Local Road	Edgerton Avenue	Local							
94	Drainage Swale	Dry Detention Pond	29800070	W. Edgerton Avenue, W. Loomis Road to S. 27th Street	Drainage Swale	Milwaukee	Southeast	Local Road	Edgerton Avenue	Local							
95	Bioswale	Biofilter	25850070	North 92nd Street, West Capitol Drive to West Hampton Avenue	bio-swale No. 1	Milwaukee	Southeast	Local Road	North 92nd Street	Local							
96	Bioswale	Biofilter	25850070	North 92nd Street, West Capitol Drive to West Hampton Avenue	bio-swale No. 2	Milwaukee	Southeast	Local Road	North 92nd Street	Local							
97	Bioswale	Biofilter	25850070	North 92nd Street, West Capitol Drive to West Hampton Avenue	bio-swale No. 3	Milwaukee	Southeast	Local Road	North 92nd Street	Local							
98	Bioswale	Biofilter	25850070	North 92nd Street, West Capitol Drive to West Hampton Avenue	bio-swale No. 4	Milwaukee	Southeast	Local Road	North 92nd Street	Local							
99	Bioswale	Biofilter	25850070	North 92nd Street, West Capitol Drive to West Hampton Avenue	bio-swale No. 5	Milwaukee	Southeast	Local Road	North 92nd Street	Local							
100	Bioswale	Biofilter	10302085	N-S Freeway - Ryan Road Interchange Ryan Rd Bridge & Ramps	Park and Ride Bioswale	Milwaukee	Southeast	IH	94	State	6/20/2023		5/1/2028	0.75	0	Located East of given coordinates.	
101	Dry Detention Pond	Dry Detention Pond	10302085	N-S Freeway - Ryan Road Interchange Ryan Rd Bridge & Ramps	Northwest detention pond	Milwaukee	Southeast	IH	94	State	6/20/2023		5/1/2028	0.75	1	White discharge in Endwall #2 should be further inspected. Aecom notified Tracy Pinkowski (DOT) of the issue on 6/22/2023.	
102	Dry Detention Pond	Dry Detention Pond	10302085	N-S Freeway - Ryan Road Interchange Ryan Rd Bridge & Ramps	Southeast detention pond	Milwaukee	Southeast	IH	94	State	6/20/2023		5/1/2028	0.75	0	Monitor Endwalls #2, #3, #4 for sediment accumulation.	
103	Bioswale	Biofilter	24000072	W Oklahoma Avenue; S 60th Street to S 49th Street	Bio-swale No. 1	Milwaukee	Southeast	Local Road	W Oklahoma Avenue	Local							
104	Bioswale	Biofilter	24000072	W Oklahoma Avenue; S 60th Street to S 49th Street	Bio-swale No. 2	Milwaukee	Southeast	Local Road	W Oklahoma Avenue	Local							
105	Bioswale	Biofilter	24000072	W Oklahoma Avenue; S 60th Street to S 49th Street	Bio-swale No. 3	Milwaukee	Southeast	Local Road	W Oklahoma Avenue	Local							
106	Bioswale	Biofilter	24000072	W Oklahoma Avenue; S 60th Street to S 49th Street	Bio-swale No. 4	Milwaukee	Southeast	Local Road	W Oklahoma Avenue	Local							
107	Bioswale	Biofilter	24000072	W Oklahoma Avenue; S 60th Street to S 49th Street	Bio-swale No. 5	Milwaukee	Southeast	Local Road	W Oklahoma Avenue	Local							
108	Bioswale	Biofilter	24000072	W Oklahoma Avenue; S 60th Street to S 49th Street	Bio-swale No. 6	Milwaukee	Southeast	Local Road	W Oklahoma Avenue	Local							
109	Bioswale	Biofilter	24000072	W Oklahoma Avenue; S 60th Street to S 49th Street	Bio-swale No. 7	Milwaukee	Southeast	Local Road	W Oklahoma Avenue	Local							
110	Bioswale	Biofilter	24000072	W Oklahoma Avenue; S 60th Street to S 49th Street	Bio-swale No. 8	Milwaukee	Southeast	Local Road	W Oklahoma Avenue	Local							
111	Bioretention area	Biofilter	27530670	Silver Spring Drive, Jackson Drive to Bette Drive	Bioretention area 250+00	Waukesha	Southeast	CTH	VV	Local							
112	Bioretention area	Biofilter	27530670	Silver Spring Drive, Jackson Drive to Bette Drive	Bioretention area 253+00	Waukesha	Southeast	CTH	VV	Local							
113	Bioretention area	Biofilter	27530670	Silver Spring Drive, Jackson Drive to Bette Drive	Bioretention area 268+00	Waukesha	Southeast	CTH	VV	Local							
114	Bioretention area	Biofilter	27530670	Silver Spring Drive, Jackson Drive to Bette Drive	Bioretention area 271+00	Waukesha	Southeast	CTH	VV	Local							
115	Bioretention area	Biofilter	27530670	Silver Spring Drive, Jackson Drive to Bette Drive	Bioretention area 275+34	Waukesha	Southeast	CTH	VV	Local							
116	Bioretention area	Biofilter	27530670	Silver Spring Drive, Jackson Drive to Bette Drive	Bioretention area 278+50	Waukesha	Southeast	CTH	VV	Local							
117	Bioretention area	Biofilter	27530670	Silver Spring Drive, Jackson Drive to Bette Drive	Bioretention area 288+50	Waukesha	Southeast	CTH	VV	Local							

SCP ID	SCP TY	Final SCP Name	PROJ_ID	PROJ_DESC	SCP_DESC	CNTY	DOT_RGN	ROAD_SYS	ROAD_N M	Maintenance Responsibility	Permit Cycle 1 (2018-2023) Inspection Date	Permit Cycle 2 (2023-2028) Inspection Date	Next Planned SCP Insp Date	PC1 Inspection Duration (hrs)	PC2 Inspection Duration (hrs)	Urgent Maint- enance	Inspection Notes / Maintenance-Repair Recommendation
118	Bioretention area	Biofilter	27530670	Silver Spring Drive, Jackson Drive to Bette Drive	Bioretention area 296+50	Waukesha	Southeast	CTH	VV	Local							
119	Bioretention area	Biofilter	27530670	Silver Spring Drive, Jackson Drive to Bette Drive	Bioretention area 299+50	Waukesha	Southeast	CTH	VV	Local							
120	Bioretention area	Biofilter	27530670	Silver Spring Drive, Jackson Drive to Bette Drive	Bioretention area 302+50	Waukesha	Southeast	CTH	VV	Local							
121	Bioretention area	Biofilter	27530670	Silver Spring Drive, Jackson Drive to Bette Drive	Bioretention area 323+00	Waukesha	Southeast	CTH	VV	Local							
122	Bioretention area	Biofilter	27530670	Silver Spring Drive, Jackson Drive to Bette Drive	Bioretention area 326+00	Waukesha	Southeast	CTH	VV	Local							
123	Bioretention area	Biofilter	27530670	Silver Spring Drive, Jackson Drive to Bette Drive	Bioretention area 329+25	Waukesha	Southeast	CTH	VV	Local							
124	Bioretention area	Biofilter	27530671	Silver Spring Drive, CTH Y to Jackson Drive	Bioretention area 185+00	Waukesha	Southeast	CTH	VV	Local							
125	Bioretention area	Biofilter	27530671	Silver Spring Drive, CTH Y to Jackson Drive	Bioretention area 188+00	Waukesha	Southeast	CTH	VV	Local							
126	Bioretention area	Biofilter	27530671	Silver Spring Drive, CTH Y to Jackson Drive	Bioretention area 191+50	Waukesha	Southeast	CTH	VV	Local							
127	Bioretention area	Biofilter	27530671	Silver Spring Drive, CTH Y to Jackson Drive	Bioretention area 193+50	Waukesha	Southeast	CTH	VV	Local							
128	Bioretention area	Biofilter	27530671	Silver Spring Drive, CTH Y to Jackson Drive	Bioretention area 196+00	Waukesha	Southeast	CTH	VV	Local							
129	Bioretention area	Biofilter	27530671	Silver Spring Drive, CTH Y to Jackson Drive	Bioretention area 198+67	Waukesha	Southeast	CTH	VV	Local							
130	Bioretention area	Biofilter	27530671	Silver Spring Drive, CTH Y to Jackson Drive	Bioretention area 203+50	Waukesha	Southeast	CTH	VV	Local							
131	Bioretention area	Biofilter	27530671	Silver Spring Drive, CTH Y to Jackson Drive	Bioretention area 205+50	Waukesha	Southeast	CTH	VV	Local							
132	Bioretention area	Biofilter	27530671	Silver Spring Drive, CTH Y to Jackson Drive	Bioretention area 207+50	Waukesha	Southeast	CTH	VV	Local							
133	Bioretention area	Biofilter	27530671	Silver Spring Drive, CTH Y to Jackson Drive	Bioretention area 211+50	Waukesha	Southeast	CTH	VV	Local							
134	Bioretention area	Biofilter	27530671	Silver Spring Drive, CTH Y to Jackson Drive	Bioretention area 221+50	Waukesha	Southeast	CTH	VV	Local							
135	Bioretention area	Biofilter	27530671	Silver Spring Drive, CTH Y to Jackson Drive	Bioretention area 226+25	Waukesha	Southeast	CTH	VV	Local							
136	Bioretention area	Biofilter	27530671	Silver Spring Drive, CTH Y to Jackson Drive	Bioretention area 233+50	Waukesha	Southeast	CTH	VV	Local							
137	Bioretention area	Biofilter	27530671	Silver Spring Drive, CTH Y to Jackson Drive	Bioretention area 236+50	Waukesha	Southeast	CTH	VV	Local							
138	Bioretention area	Biofilter	27530671	Silver Spring Drive, CTH Y to Jackson Drive	Bioretention area 242+75	Waukesha	Southeast	CTH	VV	Local							
139	Bioretention Basin	Biofilter	59940070	Monona Drive, City of Monona, (Broadway-Nichols Road)	Bioretention Basin	Dane	Southwest	CTH	BB	Local							
140	Bioswale	Biofilter	59920878	City of Madison, Mineral Point Road, South Junction Road Intersection	Bioswale	Dane	Southwest	CTH	S	Local							
141	Wet Detention Pond	Wet Detention Pond	59920878	City of Madison, Mineral Point Road, South Junction Road Intersection	Detention Pond	Dane	Southwest	CTH	S	Local							
142	Bioswale	Biofilter	27221170	Lincoln Avenue, Calhoun Road to CTH O (Moorland Road)	Bioswale 1	Waukesha	Southeast	Local Road	Avenue Lincoln	Local							
143	Bioswale	Biofilter	27221170	Lincoln Avenue, Calhoun Road to CTH O (Moorland Road)	Bioswale 2	Waukesha	Southeast	Local Road	Avenue Lincoln	Local							
144	Bioswale	Biofilter	27221170	Lincoln Avenue, Calhoun Road to CTH O (Moorland Road)	Bioswale 3	Waukesha	Southeast	Local Road	Avenue Lincoln	Local							
145	Bioswale	Biofilter	27221170	Lincoln Avenue, Calhoun Road to CTH O (Moorland Road)	Bioswale 4	Waukesha	Southeast	Local Road	Avenue Lincoln	Local							
146	Bioswale	Biofilter	27221170	Lincoln Avenue, Calhoun Road to CTH O (Moorland Road)	Bioswale 5	Waukesha	Southeast	Local Road	Avenue Lincoln	Local							
147	Bioswale	Biofilter	27221170	Lincoln Avenue, Calhoun Road to CTH O (Moorland Road)	Bioswale 6	Waukesha	Southeast	Local Road	Avenue Lincoln	Local							
148	Bioswale	Biofilter	27221170	Lincoln Avenue, Calhoun Road to CTH O (Moorland Road)	Bioswale 7	Waukesha	Southeast	Local Road	Avenue Lincoln	Local							
149	Bioswale	Biofilter	27221170	Lincoln Avenue, Calhoun Road to CTH O (Moorland Road)	Bioswale 8	Waukesha	Southeast	Local Road	Avenue Lincoln	Local							
150	Bioswale	Biofilter	27221170	Lincoln Avenue, Calhoun Road to CTH O (Moorland Road)	Bioswale 9	Waukesha	Southeast	Local Road	Avenue Lincoln	Local							
151	Bioswale	Biofilter	27221170	Lincoln Avenue, Calhoun Road to CTH O (Moorland Road)	Bioswale 10	Waukesha	Southeast	Local Road	Avenue Lincoln	Local							
152	Bioswale	Biofilter	27221170	Lincoln Avenue, Calhoun Road to CTH O (Moorland Road)	Bioswale 11	Waukesha	Southeast	Local Road	Avenue Lincoln	Local							
153	Bioswale	Biofilter	27221170	Lincoln Avenue, Calhoun Road to CTH O (Moorland Road)	Bioswale 12	Waukesha	Southeast	Local Road	Avenue Lincoln	Local							
154	Bioswale	Biofilter	27221170	Lincoln Avenue, Calhoun Road to CTH O (Moorland Road)	Bioswale 13	Waukesha	Southeast	Local Road	Avenue Lincoln	Local							
155	Bioswale	Biofilter	27221170	Lincoln Avenue, Calhoun Road to CTH O (Moorland Road)	Bioswale 14	Waukesha	Southeast	Local Road	Avenue Lincoln	Local							
156	Bioswale	Biofilter	27221170	Lincoln Avenue, Calhoun Road to CTH O (Moorland Road)	Bioswale 15	Waukesha	Southeast	Local Road	Avenue Lincoln	Local							
157	Bioswale	Biofilter	27221170	Lincoln Avenue, Calhoun Road to CTH O (Moorland Road)	Bioswale 16	Waukesha	Southeast	Local Road	Avenue Lincoln	Local							
158	Bioswale	Biofilter	27221170	Lincoln Avenue, Calhoun Road to CTH O (Moorland Road)	Bioswale 17	Waukesha	Southeast	Local Road	Avenue Lincoln	Local							
159	Bioswale	Biofilter	27221170	Lincoln Avenue, Calhoun Road to CTH O (Moorland Road)	Bioswale 18	Waukesha	Southeast	Local Road	Avenue Lincoln	Local							
160	Bioswale	Biofilter	27221170	Lincoln Avenue, Calhoun Road to CTH O (Moorland Road)	Bioswale 19	Waukesha	Southeast	Local Road	Avenue Lincoln	Local							
161	Bioswale	Biofilter	27221170	Lincoln Avenue, Calhoun Road to CTH O (Moorland Road)	Bioswale 20	Waukesha	Southeast	Local Road	Avenue Lincoln	Local							
162	Bioswale	Biofilter	27221170	Lincoln Avenue, Calhoun Road to CTH O (Moorland Road)	Bioswale 21	Waukesha	Southeast	Local Road	Avenue Lincoln	Local							
163	Bioswale	Biofilter	27221170	Lincoln Avenue, Calhoun Road to CTH O (Moorland Road)	Bioswale 22	Waukesha	Southeast	Local Road	Avenue Lincoln	Local							
164	Bioswale	Biofilter	27221170	Lincoln Avenue, Calhoun Road to CTH O (Moorland Road)	Bioswale 23	Waukesha	Southeast	Local Road	Avenue Lincoln	Local							
165	Bioswale	Biofilter	27221170	Lincoln Avenue, Calhoun Road to CTH O (Moorland Road)	Bioswale 24	Waukesha	Southeast	Local Road	Avenue Lincoln	Local							
166	Bioswale	Biofilter	27221170	Lincoln Avenue, Calhoun Road to CTH O (Moorland Road)	Bioswale 25	Waukesha	Southeast	Local Road	Avenue Lincoln	Local							
167	Bioswale	Biofilter	27221170	Lincoln Avenue, Calhoun Road to CTH O (Moorland Road)	Bioswale 26	Waukesha	Southeast	Local Road	Avenue Lincoln	Local							
168	Bioswale	Biofilter	27221170	Lincoln Avenue, Calhoun Road to CTH O (Moorland Road)	Bioswale 27	Waukesha	Southeast	Local Road	Avenue Lincoln	Local							

SCP ID	SCP TY	Final SCP Name	PROJ_ID	PROJ_DESC	SCP_DESC	CNTY	DOT_RGN	ROAD_SVS	ROAD_N M	Maintenance Responsibility	Permit Cycle 1 (2018-2023) Inspection Date	Permit Cycle 2 (2023-2028) Inspection Date	Next Planned SCP Insp. Date	PC1 Inspection Duration (hrs)	PC2 Inspection Duration (hrs)	Urgent Maint- enance	Inspection Notes / Maintenance-Repair Recommendation
169	Bioswale	Biofilter	27221170	Lincoln Avenue, Calhoun Road to CTH O (Moorland Road)	Bioswale 28	Waukesha	Southeast	Local Road	Lincoln Avenue	Local							
170	Bioswale	Biofilter	27221170	Lincoln Avenue, Calhoun Road to CTH O (Moorland Road)	Bioswale 29	Waukesha	Southeast	Local Road	Lincoln Avenue	Local							
171	Bioswale	Biofilter	27221170	Lincoln Avenue, Calhoun Road to CTH O (Moorland Road)	Bioswale 30	Waukesha	Southeast	Local Road	Lincoln Avenue	Local							
172	Bioswale	Biofilter	27221170	Lincoln Avenue, Calhoun Road to CTH O (Moorland Road)	Bioswale 31	Waukesha	Southeast	Local Road	Lincoln Avenue	Local							
173	Bioswale	Biofilter	27221170	Lincoln Avenue, Calhoun Road to CTH O (Moorland Road)	Bioswale 32	Waukesha	Southeast	Local Road	Lincoln Avenue	Local							
174	Bioswale	Biofilter	27221170	Lincoln Avenue, Calhoun Road to CTH O (Moorland Road)	Bioswale 33	Waukesha	Southeast	Local Road	Lincoln Avenue	Local							
175	Bioswale	Biofilter	27221170	Lincoln Avenue, Calhoun Road to CTH O (Moorland Road)	Bioswale 34	Waukesha	Southeast	Local Road	Lincoln Avenue	Local							
176	Bioswale	Biofilter	27221170	Lincoln Avenue, Calhoun Road to CTH O (Moorland Road)	Bioswale 35	Waukesha	Southeast	Local Road	Lincoln Avenue	Local							
177	Bioswale	Biofilter	27221170	Lincoln Avenue, Calhoun Road to CTH O (Moorland Road)	Bioswale 36	Waukesha	Southeast	Local Road	Lincoln Avenue	Local							
178	Bioswale	Biofilter	27221170	Lincoln Avenue, Calhoun Road to CTH O (Moorland Road)	Bioswale 37	Waukesha	Southeast	Local Road	Lincoln Avenue	Local							
179	Bioswale	Biofilter	27221170	Lincoln Avenue, Calhoun Road to CTH O (Moorland Road)	Bioswale 39	Waukesha	Southeast	Local Road	Lincoln Avenue	Local							
180	Bioswale	Biofilter	16933871	Hank Aaron State Trail, 33rd Court Bridge and Approaches	Upper Swale	Milwaukee	Southeast	Non-HWY	Hank Aaron State Trail	State	7/20/2023		5/1/2028	0.30		0	
181	Bioswale	Biofilter	16933871	Hank Aaron State Trail, 33rd Court Bridge and Approaches	West Swale	Milwaukee	Southeast	Non-HWY	Hank Aaron State Trail	State	7/20/2023		5/1/2028	0.25		0	
182	Rain Garden	Infiltration Basin	16933871	Hank Aaron State Trail, 33rd Court Bridge and Approaches	Middle Rain Garden	Milwaukee	Southeast	Non-HWY	Hank Aaron State Trail	State	7/20/2023		5/1/2028	0.25		0	
183	Rain Garden	Infiltration Basin	16933871	Hank Aaron State Trail, 33rd Court Bridge and Approaches	Lower Rain Garden	Milwaukee	Southeast	Non-HWY	Hank Aaron State Trail	State	7/20/2023		5/1/2028	0.25		0	
184	Bioswale	Biofilter	37380273	104th Street Village of Pleasant Prairie, Corporate Dr to E. of Terwall Terrace	Bioswale	Kenosha	Southeast	STH	165	State	7/6/2023		5/1/2028	0.25		0	
185	Rain Garden	Dry Detention Pond	69980275	Post Road, Village of Whiting, Bus 51 and Minnesota Avenue Intersection	Rain Garden 1	Portage	North Central	Bus	51	State	8/15/2022		5/1/2027			0	Secure Cover on outlet device
186	Rain Garden	Dry Detention Pond	69980275	Post Road, Village of Whiting, Bus 51 and Minnesota Avenue Intersection	Rain Garden 2	Portage	North Central	Bus	51	State	8/15/2022		5/1/2027			0	Grate should be fastened down. Animal burrow may need to be filled in
187	Rain Garden	Biofilter	59900078	City of Janesville, River Street, West Racine Street - West Court Street	North Rain Garden	Rock	Southwest	Local Road	River Street	Local							
188	Rain Garden	Biofilter	59900078	City of Janesville, River Street, West Racine Street - West Court Street	South Rain Garden	Rock	Southwest	Local Road	River Street	Local							
189	Rain Garden	Infiltration Basins	12000181	Madison - Cambridge, Dutch Mill Park & Ride Lot Expansion	Rain Garden #1	Dane	Southwest	USH	12	State	5/23/2022		5/1/2027			0	
190	Rain Garden	Infiltration Basins	12000181	Madison - Cambridge, Dutch Mill Park & Ride Lot Expansion	Rain Garden #2	Dane	Southwest	USH	12	State	5/23/2022		5/1/2027			0	Remove sediment accumulating in curb cuts.
191	Infiltration Trench	Infiltration Trench	12000181	Madison - Cambridge, Dutch Mill Park & Ride Lot Expansion	Storm water infiltration trench #1	Dane	Southwest	USH	12	State	5/23/2022		5/1/2027			0	High point along flow path could lead to flooding of bike path.
192	Infiltration Trench	Infiltration Trench	12000181	Madison - Cambridge, Dutch Mill Park & Ride Lot Expansion	Storm water infiltration trench #2	Dane	Southwest	USH	12	State	5/23/2022		5/1/2027			0	
193	Rain Garden	Dry Detention Pond	16933870	Valley Passage Connections, 37th Street and Pierce Street to Canal Street	West Garden	Milwaukee	Southeast	Non-HWY	Hank Aaron State Trail	State	7/20/2023		5/1/2028	0.25		0	
194	Rain Garden	Dry Detention Pond	16933870	Valley Passage Connections, 37th Street and Pierce Street to Canal Street	East Garden	Milwaukee	Southeast	Non-HWY	Hank Aaron State Trail	State	7/20/2023		5/1/2028	0.25		0	
195	Dry Pond	Dry Detention Pond	13301270	Mukwonago to Genesee, CTH NN to STH 59	Dry Pond	Waukesha	Southeast	STH	83	State	8/8/2023		5/1/2028	0.25		1	
196	Dry Pond	Dry Detention Pond	13301270	Mukwonago to Genesee, CTH NN to STH 59	Series of Dry Ponds	Waukesha	Southeast	STH	83	State	8/8/2023		5/1/2028	0.25		1	
197	Wet Detention Pond	Wet Detention Pond	13301270	Mukwonago to Genesee, CTH NN to STH 59	Wet Detention Pond	Waukesha	Southeast	STH	83	State	8/8/2023		5/1/2028	0.25		0	
198	Vegetated Rock Filter	Infiltration	13301270	Mukwonago to Genesee, CTH NN to STH 59	Vegetated Rock Filter 1	Waukesha	Southeast	STH	83	State	8/8/2023		5/1/2028	0.25		0	
199	Vegetated Rock Filter	Infiltration	13301270	Mukwonago to Genesee, CTH NN to STH 59	Vegetated Rock Filter 2	Waukesha	Southeast	STH	83	State	7/28/2023		5/1/2028	0.25		0	Endwall #1 needs to be unclogged of vegetation. Large population of invasive species.
200	Forebay	Infiltration Basin	13301270	Mukwonago to Genesee, CTH NN to STH 59	Forebay 2	Waukesha	Southeast	STH	83	State	7/28/2023		5/1/2028	0.25		0	Endwall #3 needs to be unclogged of vegetation. Could not access two endwalls due to abrupt elevation
201	Forebay	Infiltration Basin	13301270	Mukwonago to Genesee, CTH NN to STH 59	Forebay 1	Waukesha	Southeast	STH	83	State	7/28/2023		5/1/2028	0.25		0	Endwall #2 needs to be unclogged of vegetation.
202	Vegetative Buffer	Filter Strip	13301270	Mukwonago to Genesee, CTH NN to STH 59	Vegetative Buffer 2	Waukesha	Southeast	STH	83	State	7/28/2023		5/1/2028	0.15		0	
203	Vegetative Buffer	Filter Strip	13301270	Mukwonago to Genesee, CTH NN to STH 59	Vegetative Buffer 1	Waukesha	Southeast	STH	83	State	7/28/2023		5/1/2028	0.15		0	
204	Wet Detention Pond	Wet Detention Pond	59920982	City of Verona, North Main Street, (Cross Country Road to CTH PD)	Wet Detention Pond	Dane	Southwest	CTH	M	Local							
205	Dry Pond	Dry Detention Pond	59920982	City of Verona, North Main Street, (Cross Country Road to CTH PD)	Dry Pond	Dane	Southwest	CTH	M	Local							
206	Wet Detention Pond	Wet Detention Pond	92020773	De Pere - Suamico, CTH J - USH 41, CTH J - Duck Creek, CTH EB Ramps	Pond 18	Brown	Northeast	STH	29	State		8/19/2024	5/1/2029		0.72	0	Remove sediment and vegetation at IP5. monitor: undercutting at IP2 and IP3

SCP_ID	SCP_TY	Final SCP Name	PROJ_ID	PROJ_DESC	SCP_DESC	CNTY	DOT_RGN	ROAD_SYS	ROAD_N M	Maintenance Responsibility	Permit Cycle 1 (2018-2023) Inspection Date	Permit Cycle 2 (2023-2028) Inspection Date	Next Planned SCP Insp. Date	PC1 Inspection Duration (hrs)	PC2 Inspection Duration (hrs)	Urgent Maint- enance	Inspection Notes / Maintenance-Repair Recommendation
207	Wet Detention Pond	Wet Detention Pond	11201173	STH 26 - Breezewood Lane, STH 21 - USH 45, US 45, Fernae Avenue, Snell Road, & Stillman Drive	Pond 45C	Winnebago	Northeast	USH	41	State		6/24/2024	5/1/2029		0.50	0	Remove sediment and dense vegetation at IP3, monitor trees in riprap at IP2, monitor wild parsnip along west side of embankment
208	Wet Detention Pond	Wet Detention Pond	92020772	De Pere - Suamico, CTH J - CTH EB South Frontage Rd, CTH RK (S Frontage RD) Duck Crk - CTH EB	Pond 16	Brown	Northeast	STH	29	State		8/19/2024	5/1/2029		0.50	0	Repair undercutting at IP1 and remove vegetation at outlet device
209	Wet Detention Pond	Wet Detention Pond	10603373	Zoo IC - Swan Boulevard, Local Street, Watertown Plank Road to USH 45	Wisconsin Lutheran College Pond	Milwaukee	Southeast	Local Road	Swan Blvd	Local							
210	Wet Detention Pond	Wet Detention Pond	10603372	Zoo IC - Watertown Plank Interchange, Wisconsin Avenue to Underwood Parkway	Ramp WG Pond	Milwaukee	Southeast	USH	45	State	7/25/2023		5/1/2028	0.50		0	Noticeable erosion observed on south embankment downstream of Endwall #2
211	Wet Detention Pond	Wet Detention Pond	10603375	Zoo IC - UPRR&STH100, Bridges Ovr I94, HAST to I94 Ramp Terminal	STH 100 Loop Ramp Pond	Milwaukee	Southeast	STH	100	State	7/25/2023		5/1/2028	1.00		1	Collected sample (white discharge) from Endwall #2. The discharge was contained within the pond and
212	Dry Detention Pond	Dry Detention Pond	10603375	Zoo IC - UPRR&STH100, Bridges Ovr I94, HAST to I94 Ramp Terminal	East Depression	Milwaukee	Southeast	STH	100	State	7/25/2023		5/1/2028	0.25		0	Pipe at Endwall #1 is separating at the first joint.
213	Dry Detention Pond	Dry Detention Pond	10603375	Zoo IC - UPRR&STH100, Bridges Ovr I94, HAST to I94 Ramp Terminal	West Depression	Milwaukee	Southeast	STH	100	State	7/25/2023		5/1/2028	0.25		0	~40% area covered with phragmites. This should be monitored.
214	Wet Detention Pond	Wet Detention Pond	10603381	Zoo IC - Zoo Interchange Phase 2, Lincoln - Bluemound, 121st to 70th	Southwest Core Pond	Milwaukee	Southeast	USH	45	State	7/25/2023		5/1/2028	1.50		1	West and East embankment of the pond have major erosion issues particularly downstream of the downspouts from the bridge scuppers. Perimeter fence locked.
216	Wet Detention Pond with Forebay	Wet Detention Pond with Forebay	10310772	N-S Freeway - CTH C Interchange Mainline and Ramps	Water Quality Pond NW	Kenosha	Southeast	IH	94	State	7/6/2023		5/1/2028	0.50		0	
217	Wet Detention Pond	Wet Detention Pond	10310772	N-S Freeway - CTH C Interchange Mainline and Ramps	Water Quality Pond SE	Kenosha	Southeast	IH	94	State	7/6/2023		5/1/2028	0.25		0	
218	Wet Detention Pond	Wet Detention Pond	10310772	N-S Freeway - CTH C Interchange Mainline and Ramps	Pond NE	Kenosha	Southeast	IH	94	State	7/6/2023		5/1/2028	0.25		1	Severe undercutting of Endwall #3 and surrounding erosion must be addressed. 5 ft. deep hole present
220	Wet Detention Pond	Wet Detention Pond	10301170	N-S Fwy CTH K Interchange, E Frontage Rd/Local Rds	Water Quality Pond NE	Racine	Southeast	IH	94	State	6/15/2023		5/1/2028	0.25		0	
221	Wet Detention Pond	Wet Detention Pond	10301174	N-S Fwy CTH K Interchange, W Frontage Rd/Local Rds	Stormwater Detention Pond SW	Racine	Southeast	IH	94	State	6/15/2023		5/1/2028	0.75		1	Significant erosion and riprap displacement on northeast embankment located near commercial property.
222	Wet Detention Pond	Wet Detention Pond	10302581	N-S Freeway CTH K to CTH G, Detention Pond	Stormwater detention pond	Racine	Southeast	IH	94	State	6/15/2023		5/1/2028	0.50		0	The west embankment is eroding with visible gullies and soft spots. Monitor vegetation growth at the
223	Wet Detention Pond	Wet Detention Pond	10350370	N-S Freeway, CTH KR Interchange, East Frontage Road and West Frontage Road	Stormwater quality pond	Kenosha	Southeast	IH	94	Local							
224	Wet Detention Pond	Wet Detention Pond	27480370	Waukesha to Ackerville Road, Prospect Court to Howard Lane	CTH VV Retention Pond	Waukesha	Southeast	STH	164	State	8/8/2023		5/1/2028	0.50		0	Vegetation should be cleared near Endwall #2, #3 & #4 for easy access
225	Wet Detention Pond	Wet Detention Pond	20400170	Ryan Road, (Root River to S. 27th Street)	41st Street Detention Pond	Milwaukee	Southeast	STH	100	State	6/20/2023		5/1/2028	0.50		0	Monitor Endwall #2 and #3 for sediment and vegetation accumulation.
226	Wet Detention Pond	Wet Detention Pond	20400170	Ryan Road, (Root River to S. 27th Street)	29th Street Detention Pond	Milwaukee	Southeast	STH	100	State	6/20/2023		5/1/2028	0.25		0	
227	Wet Detention Pond with Forebay	Wet Detention Pond with Forebay	10350171	N-S Fwy 7 Mi Rd Intrchnge, East Frontage Road	NE Detention Pond	Racine	Southeast	IH	94	State	6/20/2023		5/1/2028	0.50		0	South embankment of the pond is eroding. Endwall #1 is ~50% clogged with sediment and vegetation.
229	Vegetated Rock Filter	Infiltration	10350175	N-S Fwy 7 Mi Rd Intrchnge, West Frontage Road	Vegetated Rock Filter	Racine	Southeast	IH	94	State	6/20/2023		5/1/2028	0.75		0	Inspectors were unable to walk along east embankment due to trees, standing water, and dense
230	Drainage Ditch	Grassed Swale	10302370	N-S Freeway-CTH E Interchange, West Frontage Road	Drainage Ditch A	Kenosha	Southeast	IH	94	State	6/26/2023		5/1/2028	0.25		0	
231	Drainage Ditch	Grassed Swale	10302370	N-S Freeway-CTH E Interchange, West Frontage Road	Drainage Ditch B	Kenosha	Southeast	IH	94	State	6/26/2023		5/1/2028	0.25		0	
232	Wet Detention Pond	Wet Detention Pond	10302371	N-S Freeway-CTH E Interchange, East Frontage Road	SE Retention Pond	Kenosha	Southeast	IH	94	State	6/26/2023		5/1/2028	0.75		0	Monitor Endwall #1 & #2 to ensure debris is not inhibiting flow and functionality. Debris . The outlet device with a 48" diameter manhole could not be identified
233	Wet Detention Pond	Wet Detention Pond	10302371	N-S Freeway-CTH E Interchange, East Frontage Road	NE Retention Pond	Kenosha	Southeast	IH	94	State	6/26/2023		5/1/2028	0.50		0	Endwall #2 is not accessible due to dense vegetation.
234	Wet Detention Pond	Wet Detention Pond	10302571	N-S Freeway - CTH G Interchange, CTH G, E and W Frontage Rds	Detention Pond NW	Racine	Southeast	IH	94	State	6/15/2023		5/1/2028	1.50		0	Limited access to the north end of the pond due to fence and tall vegetation.
235	Wet Detention Pond	Wet Detention Pond	10302571	N-S Freeway - CTH G Interchange, CTH G, E and W Frontage Rds	Detention Pond SE	Racine	Southeast	IH	94	State	6/15/2023		5/1/2028	0.50		0	Soft spots and bare spots present on the west embankment of the pond.
236	Wet Detention Pond	Wet Detention Pond	11200971	STH 26 - Breezewood Lane, USH 45 - Breezewood Lane, USH 41 Mainline	CTH G Pond	Winnebago	Northeast	USH	41	State	6/16/2022		5/1/2027			0	Vegetation growing downstream of Endwall #4 and Outlet is restricting flow in/out of pond. Algae growing in standing water. Endwalls on Outlet have failed off and need to be replaced.
237	Wet Detention Pond with Forebay	Wet Detention Pond with Forebay	12220973	Sheboygan - Manitowoc, (Sheboygan River - STH 23)	Retention Pond	Sheboygan	Northeast	IH	43	Local		8/7/2024	5/1/2029	0.62		0	Hans mentioned this L Shaped Pond is a Local Pond, not State Maintained on 12/10/2024.
240	Retired																
241	Drainage Ditch	Trench	12220973	Sheboygan - Manitowoc, (Sheboygan River - STH 23)	IH 43 South Drainage Ditch	Sheboygan	Northeast	IH	43	State		8/7/2024	5/1/2029		0.25	0	No Comments
242	Wet Detention Pond	Wet Detention Pond	11201175	STH 26 - Breezewood Lane, STH 21 - USH 45, STH 21 Interchange & Lake	Pond 21-A	Winnebago	Northeast	USH	41	State	6/16/2022		5/1/2027			0	Endwall #5 and Outlet Device #2 need to be unclogged to allow flow.
243	Wet Detention Pond	Wet Detention Pond	11201175	STH 26 - Breezewood Lane, STH 21 - USH 45, STH 21 Interchange & Lake	Pond 21-D	Winnebago	Northeast	USH	41	State		8/1/2024	5/1/2029		0.63	0	Remove sediment at IP1, repair geotextile fabric at IP7. Monitor: joint separation at IP2, sediment buildup at IP4, undercutting at IP5
244	Swale	Grass Swale	10321072	N-S Freeway-STH 50 Interchange, Mainline and Ramps	Stormwater Quality Swale	Kenosha	Southeast	IH	94	State	8/10/2023		5/1/2028	0.25		0	Endwall #1 needs to be cleared of sediment to ensure the smooth flow of water.
245	Dry Detention Pond	Dry Detention Pond	10321472	N-S Freeway-STH 158 Interchange, Mainline and Ramps	Dry Detention Pond	Kenosha	Southeast	IH	94	State	6/26/2023		5/1/2028	0.75		0	Latitude and longitude provided was located South of actual basin location.
246	Wet Detention Pond	Wet Detention Pond	13700071	Watertown Bypass (CTH R Intersection)	Sedimentation Pond	Dodge	Southwest	STH	16	State		5/15/2024	5/1/2029		0.50	0	Work needed on IP1 obstruction and erosion. Monitor erosion and sediment at IP2 and IP3
247	Infiltration Basin	Infiltration Basin	11900083	Eau Claire - Chippewa Falls Road, Eau Claire River - Hiawatha Street	Sedimentation Pond	Eau Claire	Northwest	USH	53	State		8/14/2024	5/1/2029		0.58	0	Remove sediment and debris covering grate at Outlet Device.
248	Wet Detention Pond with Forebay	Wet Detention Pond with Forebay	11900083	Eau Claire - Chippewa Falls Road, Eau Claire River - Hiawatha Street	Retention Pond	Eau Claire	Northwest	USH	53	State		8/14/2024	5/1/2029		0.78	0	Remove vegetation growth in IP5. Monitor: tree growth in IP1, apron crack in IP1.
252	Retired																
253	Infiltration Basin	Infiltration Basin	10051076	Illinois State Line - Madison, CTH O to USH 14 Interchange North	Spring Brook Infiltration Basin	Rock	Southwest	IH	39	State	6/1/2021		5/1/2026				
254	Dry Pond	Dry Detention Pond	50800177	Winsted Street, Village of Spring Green, (Hoxie Street - Baker Street)	Dry Pond	Sauk	Southwest	STH	23	Local		5/22/2024	5/1/2029	0.30		0	2024 Designated as Local: monitor sediment and vegetation at inflow point 1
255	Wet Detention Pond with Forebay	Wet Detention Pond with Forebay	50800177	Winsted Street, Village of Spring Green, (Hoxie Street - Baker Street)	Deep Pond	Sauk	Southwest	STH	23	Local		5/22/2024	5/1/2029	0.75		0	2024 Designated as Local: Work needed on the embankment erosion. Monitor erosion, undercutting, and vegetation at inflow point 2
257	Dry Detention Pond	Dry Detention Pond	10660275	Madison - Milwaukee Road, (CTH N Interchange)	East Dry Pond	Dane	Southwest	IH	94	State	8/1/2022		5/1/2027			2	Standing water in basin indicates that the basin is not functioning properly. Endwalls #4, 5, 6, and 8 need
258	Dry Detention Pond	Dry Detention Pond	10660275	Madison - Milwaukee Road, (CTH N Interchange)	West Dry Pond	Dane	Southwest	IH	94	State	8/1/2022		5/1/2027			0	
259	Dry Pond	Dry Detention Pond	10603370	Zoo IC - Mayfair Road, IH 94 to Watertown Plank Road	Dry Pond	Milwaukee	Southeast	STH	100	State	7/25/2023		5/1/2028	0.25		0	Main basin area is ~100% covered with catatils. This should be controlled.
260	Wet Detention Pond with Forebay	Wet Detention Pond with Forebay	10603370	Zoo IC - Mayfair Road, IH 94 to Watertown Plank Road	Hotel Pond	Milwaukee	Southeast	STH	100	State	7/25/2023		5/1/2028	0.50		0	Vegetation should be controlled to locate Endwall #2. Replace riprap and fabric near Endwall #1.
261	Wet Detention Pond with Forebay	Wet Detention Pond with Forebay	10603370	Zoo IC - Mayfair Road, IH 94 to Watertown Plank Road	Mayfair Pond	Milwaukee	Southeast	STH	100	State	7/25/2023		5/1/2028	0.50		0	Noticeable erosion on southeast embankment and downstream of Endwall #3. Fill holes and restore riprap.
262	Dry Pond	Dry Detention Pond	27880072	West Waukesha Bypass, Summit Ave to Northview Rd	Dry Pond 324	Waukesha	Southeast	CTH	TT	State	8/14/2023		5/1/2028	0.50		0	
263	Swale	Infiltration Basin	27880072	West Waukesha Bypass, Summit Ave to Northview Rd	Grass Treatment Swale	Waukesha	Southeast	CTH	TT	State	8/14/2023		5/1/2028	0.50		0	
264	Dry Pond	Dry Detention Pond	83130172	City of Barron Industrial Park, Watermain/Sewer/Street Improvements	West Storm Water Pond	Barron	Northwest	Local Road	Guy Ave	Local							
265	Dry Pond	Dry Detention Pond	83130172	City of Barron Industrial Park, Watermain/Sewer/Street Improvements	East Storm Water Pond	Barron	Northwest	Local Road	Guy Ave	Local							
266	Dry Pond	Dry Detention Pond	37380670	104th Street CTH ML and CTH EZ Intersections	Eco-Pond	Kenosha	Southeast	STH	165	State	7/6/2023		5/1/2028	0.25		0	Pipe located at Endwall #2 is 50% clogged with sediment and vegetation. This requires immediate
267	Wet Detention Pond with Forebay	Wet Detention Pond with Forebay	37380670	104th Street CTH ML and CTH EZ Intersections	Storm Water Pond 1	Kenosha	Southeast	STH	165	State	7/6/2023		5/1/2028	0.50		0	Scours present on the south embankment of the pond near the forebay. Endwall #2 is 75% clogged with sediment and vegetation.
269	Dry Pond	Dry Detention Pond	30300870	Summit Avenue, City of Oconomowoc, (Blue Ribbon Drive - Thackeray Trail)	Pond #1	Waukesha	Southeast	STH	67	State	7/5/2023		5/1/2028	0.50		0	Endwall #3 and #4 need to be unclogged of vegetation

SCP ID	SCP TY	Final SCP Name	PROJ_ID	PROJ_DESC	SCP_DESC	CNTY	DOT_RGN	ROAD_SYS	ROAD_N M	Maintenance Responsibility	Permit Cycle 1 (2018-2023) Inspection Date	Permit Cycle 2 (2023-2028) Inspection Date	Next Planned SCP Insp. Date	PC1 Inspection Duration (hrs)	PC2 Inspection Duration (hrs)	Urgent Maint- enance	Inspection Notes / Maintenance-Repair Recommendation
322	Wet Detention Pond	Wet Detention Pond	11330377	DePere - Suamico, Morris Avenue - Memorial Drive, Morris Ave - Ninth St Mainline	Pond 8	Brown	Northeast	USH	41	State		6/27/2024	5/1/2029		0.75	1	2024 Urgent Maintenance: Remove veg, trash, and sediment from IP2.WN: remove sediment and debris from IP1, remove sediment from IP3, remove veg, debris from IP5, remove sediment and dense veg. at IP6,
323	Retired																
324	Retired																
325	Retired																
326	Wet Detention Pond	Wet Detention Pond	15170772	USH 10 - USH 10/STH 441, County CB - Oneida Street, USH 41 Interchange Phase 1	Pond 1	Winnebago	Northeast	USH	10	State	6/16/2022		5/1/2027			0	
327	Drainage Ditch	Grassed Swale	15170772	USH 10 - USH 10/STH 441, County CB - Oneida Street, USH 41 Interchange Phase 1	Ditch 1	Winnebago	Northeast	USH	10	State	6/16/2022		5/1/2027			0	Trash and debris accumulating in Endwall #2 is impeding flow and needs to be removed
328	Drainage Ditch	Grassed Swale	15170772	USH 10 - USH 10/STH 441, County CB - Oneida Street, USH 41 Interchange Phase 1	Ditch 2	Winnebago	Northeast	USH	10	State	6/16/2022		5/1/2027			0	
329	Wet Detention Pond	Wet Detention Pond	15177575	USH 10 - USH 10/STH 441, County CB - Onedia Street, Racine Rd (CTH P) Intchg	Pond 2	Winnebago	Northeast	USH	10	State	6/16/2022		5/1/2027			0	
330	Wet Detention Pond	Wet Detention Pond	15177575	USH 10 - USH 10/STH 441, County CB - Onedia Street, Racine Rd (CTH P) Intchg	Pond 3	Winnebago	Northeast	USH	10	State	6/16/2022		5/1/2027			0	Erosion downstream of Endwalls #1, #2, and #3. Rip rap needs to be extended.
331	Wet Detention Pond	Wet Detention Pond	15170774	USH 10 - USH 10/STH 441, County CB - Oneida Street, I-41 (STH 441 - CTH II)	Pond 6	Winnebago	Northeast	USH	10	State	6/16/2022		5/1/2027			0	Heavy erosion along sides of outlet grate #1. Heavy vegetation accumulated on outlet grate #2.
332	Wet Detention Pond	Wet Detention Pond	15170774	USH 10 - USH 10/STH 441, County CB - Oneida Street, I-41 (STH 441 - CTH II)	Pond 7	Winnebago	Northeast	USH	10	State	6/16/2022		5/1/2027			0	Only access point is near the overpass.
333	Wet Detention Pond	Wet Detention Pond	12060774	Mount Horeb - Madison, CTH PD to Raymond Rd, Stg 2/Phase 1	Pond D	Dane	Southwest	USH	18	Local	6/15/2022		5/1/2027			0	
334	Infiltration Basin	Wet Detention Pond	12060774	Mount Horeb - Madison, CTH PD to Raymond Rd, Stg 2/Phase 1	Pond A	Dane	Southwest	USH	18	Local	6/15/2022		5/1/2027			0	Emergency spillways are submerged and no longer functioning as designed.
335	Swale	Grass Swale	10710682	La Crosse - Galesville, Livingston ST to West George St	Treatment Swale	La Crosse	Southwest	USH	53	State			5/1/2025	0.00			Please inspect - Hans_7.21.2025
336	Wet Detention Pond	Wet Detention Pond	59920640	East Washington Ave, City of Madison, (Marquette Street to Melvin Court)	Retention Pond	Dane	Southwest	USH	151	Connecting Hwy				0.00			
337	Wet Detention Pond with Forebay	Wet Detention Pond with Forebay	33600970	Menomonee Falls - Slinger Road, (Beechwood Industrial Ct to Polk Street)	AWSMS Retention Pond	Washington	Southeast	STH	175	State	8/6/2024		5/1/2029		0.35	0	No Comments
338	Wet Detention Pond with Forebay	Wet Detention Pond with Forebay	13710870	W. Wisconsin Avenue, Oconomowoc Bypass to S. Concord Road	Retention Pond	Waukesha	Southeast	STH	16	State	7/19/2023		5/1/2028	0.30		0	Entire pond including endwalls have serious algae growth.
339	Wet Detention Pond	Wet Detention Pond	66510171	USH 51/STH 29 Corridor - Wausau, Lily Lane - Daffodil Lane	Detention Pond	Marathon	North Central	CTH	N	Local							
340	Dry Pond	Dry Detention Pond with	75900072	Eleva - Eau Claire Road, (Walnut Road - CTH II)	Retention Pond	Eau Claire	Northwest	STH	93	State		8/13/2024	5/1/2029		0.25	0	No Comments
342	Drainage Channel	Grassed Swale	33300370	Green Bay Road, (Four Mile Road - Six Mile Road)	Channel	Racine	Southeast	STH	31	State		6/6/2025	5/1/2025	0.00			
343	Wet Detention Pond	Wet Detention Pond	11660771	USH 51/STH 29 Corridor - Wausau, CTH NN Structure Over USH 51/STH 29	CTH NN NE Stormwater Pond	Marathon	North Central	USH	51	State	8/15/2022		5/1/2027			0	Secure Cover on outlet device. Secure cover on end wall #1
344	Dry Detention Pond	Dry Detention Pond	10302274	N-S Freeway, STH 142 Interchange, West Frontage Road	NW Dry Detention Pond	Kenosha	Southeast	IH	94	State	7/17/2023		5/1/2028	0.25		0	Endwall #2 (~50% clogged) should be cleared of sediment
345	Dry Pond	Trench	51200771	La Crosse - Cashton, East City Limits - Boma Road	Horseshoe Pond	La Crosse	Southwest	STH	33	State		6/18/2024	5/1/2029		0.50	0	Work needed: remove sediment at IP1, Repair separated joint at IP3, remove vegetation at IP4, remove standing water in main basin, clean out sediment in main basin, restore rilling along main basin.
346	Wet Detention Pond	Wet Detention Pond	51200771	La Crosse - Cashton, East City Limits - Boma Road	Knobloch Pond	La Crosse	Southwest	STH	33	State		6/18/2024	5/1/2029		0.50	0	Remove cattails blocking flow at outlet device. Monitor sediment accumulation downstream of IP1,
347	Wet Detention Pond	Wet Detention Pond	11330370	DePere - Suamico, Orange Lane-CTH M, Salt Storage Facility	Detention Basin	Brown	Northeast	Off System		State		10/14/2024	5/1/2029		0.50	0	Work Needed: Remove sediment accumulated in the 15" RCP outlet. Monitor: Vegetation and large animal burrow on southern embankment.
348	Wet Detention Pond	Wet Detention Pond	29870074	5th Avenue Relocation, STH 32 to Ryan Road	Wet Detention Pond A	Milwaukee	Southeast	Local Road	5th Avenue	Local							
349	Wet Detention Pond	Wet Detention Pond	29870074	5th Avenue Relocation, STH 32 to Ryan Road	Wet Detention Pond B	Milwaukee	Southeast	Local Road	5th Avenue	Local							
350	Wet Detention Pond with Forebay	Wet Detention Pond with Forebay	31800870	Burlington Bypass, STH 83 (South) to STH 36/83 (North) Grading	Detention Pond #411	Racine	Southeast	STH	11	State	7/17/2023		5/1/2028	0.75		0	The triple endwall (Endwall#4, #5 & #6) is either not installed as per the original plan or has been removed.
351	Wet Detention Pond with Forebay	Wet Detention Pond with Forebay	31800870	Burlington Bypass, STH 83 (South) to STH 36/83 (North) Grading	Detention Pond #415B	Racine	Southeast	STH	11	State	7/17/2023		5/1/2028	0.75		0	Monitor Endwalls (#4 & #5) which are located near the forebay has sediment and standing water. Monitor Endwall #2 & #4 for vegetation accumulation.
352	Dry Detention Pond	Dry Detention Pond	27510670	CTH Q (County Line Road), USH 41/45 to Prilgrim Road	West Detention Pond	Washington	Southeast	Local Road		County Line Road							
353	Wet Detention Pond	Wet Detention Pond	27510670	CTH Q (County Line Road), USH 41/45 to Prilgrim Road	East Detention Pond	Washington	Southeast	Local Road		County Line Road							
354	Wet Detention Pond	Wet Detention Pond	11330676	DePere - Suamico, Stormwater Detention Pond	Wet Storm water detention pond	Brown	Northeast	Off System		Private			5/1/2025	0.00			
355	Dry Detention Pond	Dry Detention Pond	22300170	Greenfield Avenue, STH 164 to Barker Road (CTH Y)	Detention Basin 117+00	Waukesha	Southeast	STH	59	State	8/1/2023		5/1/2028	0.25		0	Clear the sediment and vegetation from the pipe at Endwall #4 & #6
356	Infiltration Basin	Infiltration Basin	22300170	Greenfield Avenue, STH 164 to Barker Road (CTH Y)	Detention Basin 119+00	Waukesha	Southeast	STH	59	State	8/1/2023		5/1/2028	0.25		0	Monitor standing water near Endwall #2 during future inspections. No riprap observed near Endwall #1 &
357	Swale	Infiltration Basin	22300170	Greenfield Avenue, STH 164 to Barker Road (CTH Y)	Stormwater swale 122+50	Waukesha	Southeast	STH	59	State	8/1/2023		5/1/2028	0.25		0	Clear the sediment and vegetation from Endwall #1
358	Swale	Infiltration Basin	22300170	Greenfield Avenue, STH 164 to Barker Road (CTH Y)	Detention swale 155+00	Waukesha	Southeast	STH	59	State	8/1/2023		5/1/2028	0.75		0	Vegetation should be cleared to access the endwall #5. Standing water near Endwall #1 & #6 should be
359	Swale	Infiltration Basin	22300170	Greenfield Avenue, STH 164 to Barker Road (CTH Y)	Stormwater swale 135+00	Waukesha	Southeast	STH	59	State	8/1/2023		5/1/2028	0.25		0	The sediment should be cleared from the endwall to ensure the proper flow of stormwater
360	Dry Detention Pond	Dry Detention Pond	20750470	Nicholson Ave./Pennsylvania Ave., Rawson Av/CTH BB - College Ave/CTH ZZ	Detention Pond	Milwaukee	Southeast	Local Road		Nicholson Ave							
361	Wet Detention Pond	Wet Detention Pond	29790170	CTH ZZ, W. Loomis road - S. 51st Street	Detention Pond #1	Milwaukee	Southeast	CTH	ZZ	Local							
362	Wet Detention Pond	Wet Detention Pond	29790170	CTH ZZ, W. Loomis road - S. 51st Street	Detention Pond #2	Milwaukee	Southeast	CTH	ZZ	Local							
363	Wet Detention Pond	Wet Detention Pond	31330470	East Troy, North Street, West Village Limits to East of Thomas Lane	Detention Basin	Walworth	Southeast	STH	20	State		6/4/2024	5/1/2029		0.50	0	6" sediment blocking flow at outlet device 1.
364	Dry Detention Pond	Dry Detention Pond	10302080	N-S Freeway, College Avenue Interchange, College Avenue Ramps and Local Cross Road	Northwest detention pond	Milwaukee	Southeast	IH	94	State	7/13/2023		5/1/2028	1.00		0	Vegetation growth at endwalls (#4 & #6)
365	Dry Detention Pond	Dry Detention Pond	10302080	N-S Freeway, College Avenue Interchange, College Avenue Ramps and Local Cross Road	Southeast detention pond	Milwaukee	Southeast	IH	94	State	7/13/2023		5/1/2028	0.75		0	Vegetation & erosion at endwalls.
366	Dry Detention Pond	Dry Detention Pond	10302082	N-S Freeway, Rawson Ave Interchange, Rason Ave & Ramps	Southeast dry detention pond	Milwaukee	Southeast	IH	94	State	7/13/2023		5/1/2028	0.75		1	5-foot deep hole along Rawson Rd.
367	Wet Detention Pond	Wet Detention Pond	59920931	City of Madison, McKee Road, Meriter Way to Maple Grove Drive	Pond A	Dane	Southwest	CTH	PD	Local							
368	Wet Detention Pond	Wet Detention Pond	59920931	City of Madison, McKee Road, Meriter Way to Maple Grove Drive	Pond B	Dane	Southwest	CTH	PD	Local							
369	Wet Detention Pond with Forebay	Wet Detention Pond with Forebay	31500270	Drainage Improvement, Village of Darien	Detention Pond	Walworth	Southeast	USH	14	State		5/30/2024	5/1/2029		0.50	0	Monitor west side outlet device
370	Wet Detention Pond with Forebay	Wet Detention Pond with Forebay	40300470	East Green Bay Avenue/West Grand Avenue, I-43 to South Tower Drive	Wet Detention Pond	Ozaukee	Southeast	STH	33	Connecting Hwy			5/1/2025	0.00			Confirmed Connecting Hwy with Hans on 04/29/2025

SCP ID	SCP TY	Final SCP Name	PROJ_ID	PROJ_DESC	SCP_DESC	CNTY	DOT_RGN	ROAD_SVS	ROAD_N M	Maintenance Responsibility	Permit Cycle 1 (2018-2023) Inspection Date	Permit Cycle 2 (2023-2028) Inspection Date	Next Planned SCP Insp. Date	PC1 Inspection Duration (hrs)	PC2 Inspection Duration (hrs)	Urgent Maint- enance	Inspection Notes / Maintenance-Repair Recommendation
371	Biofilter	Biofilter	92101171	Oneida - Green Bay, CTH U - STH 172	West Detention Pond	Brown	Northeast	STH	54	State		7/24/2024	5/1/2029		0.13	0	Remove trees above endwall. Monitor: tree downstream of IP1, invasive vegetation.
372	Biofilter	Biofilter	92101171	Oneida - Green Bay, CTH U - STH 172	North Detention Pond with	Brown	Northeast	STH	54	State		7/24/2024	5/1/2029		0.50	0	Monitor stability of inlet grates.
373	Biofilter	Biofilter	92101171	Oneida - Green Bay, CTH U - STH 172	East Detention Pond with Forebay	Brown	Northeast	STH	54	State		7/24/2024	5/1/2025		0.50	1	2024 Urgent Maintenance: Inspect potential illicit discharge. Monitor: sediment buildup at IP1, IP2, and IP3. Monitor cattail % cattail growth. Monitor large tree between East Forebay 2 and main basin; Reinspect
374	Dry Detention Pond	Dry Detention Pond	27221070	Coffee Road, Calhoun Road to National Avenue	Detention Pond	Waukesha	Southeast	Local Road	Coffee Road	Local							
375	Wet Detention Pond	Wet Detention Pond	11700170	Wausau - Merrill, CTH K/B51 Interchange	Pond A	Marathon	North Central	USH	51	State	8/15/2022		5/1/2027			2	Algae in main pool and bubbly-swirling water in endwall #4 are concerning
376	Wet Detention Pond	Wet Detention Pond	11700170	Wausau - Merrill, CTH K/B51 Interchange	Pond B	Marathon	North Central	USH	51	State	8/15/2022		5/1/2027			0	
377	Wet Detention Pond	Wet Detention Pond	22300270	Greenfield Avenue, Baythorn Way to Calhoun Road, (City of New Berlin, City of Brookfield, Town of Brookfield)	Detention Basin 3	Waukesha	Southeast	STH	59	Connecting Hwy	8/1/2023		8/1/2023	0.00		0	
378	Wet Detention Pond	Wet Detention Pond	22300270	Greenfield Avenue, Baythorn Way to Calhoun Road, (City of New Berlin, City of Brookfield, Town of Brookfield)	Detention Basin 1	Waukesha	Southeast	STH	59	Connecting Hwy	8/1/2023		8/1/2023	0.00		0	
379	Wet Detention Pond	Wet Detention Pond	22651670	27th St, City of Franklin/Oak Creek, W Drexel Ave to College Ave (CTH ZZ)	City of Oak Creek Pond	Milwaukee	Southeast	STH	241	State	7/13/2023		5/1/2028	0.75		0	The east embankment is eroding with visible gullies and soft spots.
380	Dry Detention Pond	Dry Detention Pond	37510070	CTH W, 2300 Feet South of 82nd Street to 82nd Street	Detention Pond	Kenosha	Southeast	CTH	W	Local							
381	Dry Detention Pond	Dry Detention Pond	66750070	USH 51/STH 29 Corridor- Wausau, West Arterial Frontage Road, Oriole Lane- CTH NN	South Detention Pond	Marathon	North Central	CTH	R	Local							
382	Wet Detention Pond	Wet Detention Pond	66750070	USH 51/STH 29 Corridor- Wausau, West Arterial Frontage Road, Oriole Lane- CTH NN	North Detention Pond	Marathon	North Central	CTH	R	Local							
383	Wet Detention Pond	Wet Detention Pond	13900485	Janesville-Fort Atkinson Road, (STH 59 Relocation)	Shumway Pond	Rock	Southwest	STH	26	Connecting Hwy				0.00			
384	Wet Detention Pond	Wet Detention Pond	13900485	Janesville-Fort Atkinson Road, (STH 59 Relocation)	Pond 2	Rock	Southwest	STH	26	State		5/30/2024	5/1/2029		0.50	0	6" sediment and dense cattails locking endwall should be removed at inflow point 1 and outlet device.
385	Wet Detention Pond	Wet Detention Pond	13900485	Janesville-Fort Atkinson Road, (STH 59 Relocation)	Pond 3	Rock	Southwest	STH	26	State		5/30/2024	5/1/2029		0.30	0	Animal burrows and anthills to be monitored, erosion and deep rilling on embankment, remove vegetation
386	Wet Detention Pond	Wet Detention Pond	13900485	Janesville-Fort Atkinson Road, (STH 59 Relocation)	Pond 4	Rock	Southwest	STH	59	State		5/30/2024	5/1/2029		0.75	1	2024 Urgent Maintenance: Stabilize endwall at inflow point 2. Address damage to endwall at IP2, work
387	Wet Detention Pond	Wet Detention Pond	11660972	USH 51/STH 29 Corridor - Wausau, STH 52 Extension, Seymour Lane to 18th Avenue	Pond A	Marathon	North Central	STH	29	Connecting Hwy				0.00			
388	Wet Detention Pond	Wet Detention Pond	11660972	USH 51/STH 29 Corridor - Wausau, STH 52 Extension, Seymour Lane to 18th Avenue	Pond B	Marathon	North Central	STH	29	Connecting Hwy				0.00			
389	Wet Detention Pond	Wet Detention Pond	11661071	USH 51/STH 29 Corridor - Wausau, STH 29/STH 52 Extension, 39th Avenue to Seymour Lane	Pond D	Marathon	North Central	STH	29	Connecting Hwy				0.00			
390	Dry Detention Pond	Dry Detention Pond	11661174	USH 51/STH 29 Corridor - Wausau, USH 51 Southbound, Sherman Street to STH 52	Pond C	Marathon	North Central	USH	51	State	8/15/2022		5/1/2027			0	
391	Dry Detention Pond	Dry Detention Pond	22300670	Greenfield Avenue, Barker Road (CTH Y) to Baythorn Way, (City of New Berlin, City of Brookfield, Town of Brookfield)	Detention Basin 195+00	Waukesha	Southeast	STH	59	State	8/1/2023		5/1/2028	0.25		0	Vegetation near Endwall #1 & #2 should be monitored.
392	Wet Detention Pond	Wet Detention Pond	27230071	Moorland Road, Woods Road to Janesville Road	Detention Basin	Waukesha	Southeast	Local Road	Moorland Road	Local							
393	Wet Detention Pond	Wet Detention Pond	14401071	Coary Lane - CTH OJ, WB / WCL RR & Mullett, River Structures	Detention Pond	Sheboygan	Northeast	STH	23	State		8/7/2024	5/1/2029		0.53	0	Standing water at IP 1 (~1/4 of RCP)
394	Retired																
395	Wet Detention Pond	Wet Detention Pond	12060777	Mount Horeb-Madison, Raymond Road to Seminole Highway, Phase I	Detention Basin A	Dane	Southwest	USH	18	State	6/15/2022		5/1/2027			0	Trash and debris in endwall #2 is impeding flow and needs to be removed.
396	Wet Detention Pond	Wet Detention Pond	12060777	Mount Horeb-Madison, Raymond Road to Seminole Highway, Phase I	Detention Basin B	Dane	Southwest	USH	18	State	6/15/2022		5/1/2027			0	Endwall #2 and Outlet #2 need to be unclogged. Overland flow needs additional rip rap due to erosion and gully forming.
397	Wet Detention Pond with Forebay	Wet Detention Pond with Forebay	11660670	USH 51/STH 29 Corridor - Wausau, Lily Lane to Oriole Lane, CTH N & USH 51/STH 29 Interchange	SE Detention Pond	Marathon	North Central	USH	51	State	8/15/2022		5/1/2027			0	
398	Wet Detention Pond	Wet Detention Pond	16740072	Lake Delton-Sauk City Road, (IH 90/94-Terrytown Road)	Detention Pond A	Sauk	Southwest	USH	12	State	7/25/2022		5/1/2027			0	Vegetation downstream of Endwall #1 is thick and should be thinned out. As of now, water has created a
399	Dry Detention Pond	Dry Detention Pond	16740072	Lake Delton-Sauk City Road, (IH 90/94-Terrytown Road)	Detention Pond B	Sauk	Southwest	USH	12	State	8/15/2022		5/1/2027			0	
400	Dry Detention Pond	Dry Detention Pond	16740072	Lake Delton-Sauk City Road, (IH 90/94-Terrytown Road)	Detention Pond C	Sauk	Southwest	USH	12	State	8/15/2022		5/1/2027			0	Endwalls #1 and #4 need to be unclogged
401	Dry Detention Pond	Dry Detention Pond	16740072	Lake Delton-Sauk City Road, (IH 90/94-Terrytown Road)	Detention Pond D	Sauk	Southwest	USH	12	State	8/15/2022		5/1/2027			0	Remove tree in endwall #3 to prevent future structural deficiency.
402	Wet Detention Pond	Wet Detention Pond	16740072	Lake Delton-Sauk City Road, (IH 90/94-Terrytown Road)	Detention Pond E	Sauk	Southwest	USH	12	State	7/25/2022		5/1/2027			0	Add rip rap at outfall of endwall #1 to prevent future scouring
403	Wet Detention Pond	Wet Detention Pond	59890177	Gateway Boulevard, City of Beloit, Eagles Ridge Drive-Hart Road	Pond A	Rock	Southwest	Local Road	Gateway Boulevard	Local							
404	Wet Detention Pond	Wet Detention Pond	59890177	Gateway Boulevard, City of Beloit, Eagles Ridge Drive-Hart Road	Pond B	Rock	Southwest	Local Road	Gateway Boulevard	Local							
405	Wet Detention Pond	Wet Detention Pond	59890177	Gateway Boulevard, City of Beloit, Eagles Ridge Drive-Hart Road	Pond C	Rock	Southwest	Local Road	Gateway Boulevard	Local							
406	Wet Detention Pond	Wet Detention Pond	59890177	Gateway Boulevard, City of Beloit, Eagles Ridge Drive-Hart Road	Pond D	Rock	Southwest	Local Road	Gateway Boulevard	Local							
407	Wet Detention Pond	Wet Detention Pond	90720371	USH 41 - STH 64, Roosevelt Road, City of Marinette	Detention Pond	Marinette	Northeast	CTH	T	Local							
408	Dry Detention Pond	Dry Detention Pond	10302270	N-S Freeway, STH 142 Interchange, East Frontage Road	SE Dry Detention Pond	Kenosha	Southeast	IH	94	State	6/26/2023		5/1/2028	0.50		0	
409	Wet Detention Pond	Wet Detention Pond	13900474	Jefferson Bypass, STH 89 - Jahn Road	Detention Basin 1	Jefferson	Southwest	STH	26	State		5/6/2024	5/1/2029		0.75	0	Clean out sediment at IP2, Mow woody vegetation at SW embankment. Monitor standing water at IP 6,
410	Wet Detention Pond	Wet Detention Pond	58860202	Village of Waunakee and Town of Westport, Bicycle/Pedestrian Accommodations, Woodland Drive Improvements	Detention Basin	Dane	Southwest	Local Road	Woodland Drive	Local							
411	Drainage Swale	Grass Swale	58860202	Village of Waunakee and Town of Westport, Bicycle/Pedestrian Accommodations, Woodland Drive Improvements	Drainage Swale	Dane	Southwest	Local Road	Woodland Drive	Local							
412	Wet Detention Pond	Wet Detention Pond	11960075	Minong - Solon Springs, CTH T to CTH Y (SB)	Detention Pond	Douglas	Northwest	USH	53	State		6/24/2025	5/1/2025	0.00			
413	Infiltration Basin	Infiltration Basin	16740070	Lake Delton - Sauk City Road, Fer Dell Road - W Frontage Road	Infiltration Basin	Sauk	Southwest	USH	12	State	7/25/2022		5/1/2027			1	Animal burrow in basin needs to be filled
414	Dry Detention Pond	Dry Detention Pond	15177578	USH 10 - USH 10 / STH 441, County CB-Oneida Street, Appleton Rd. (STH 47) Interchange	West Dry Detention Pond	Winnebago	Northeast	USH	10	State		6/24/2024	5/1/2029		0.42	0	Repair erosion on west side embankment, Monitor: tree at IP1, cattails in main pool, standing water in main pool, tree at IP2, cattails near outlet device
415	Dry Detention Pond	Dry Detention Pond	15177578	USH 10 - USH 10 / STH 441, County CB-Oneida Street, Appleton Rd. (STH 47) Interchange	East Dry Detention Pond	Winnebago	Northeast	USH	10	State		6/24/2024	5/1/2029		0.33	0	Repair vegetation at IP1, monitor: joint sep at IP2, cattails at main basin, joint sep. at outlet structure
416	Infiltration Basin	Infiltration Basin	10700071	La Crosse - Sparta, Safety Rest Area No. 31	Basin A West	La Crosse	Southwest	IH	90	State		6/18/2024	5/1/2029		0.25	0	Monitor sediment buildup at IP2, Monitor potential erosion downstream of IP3, Monitor bare spots on
417	Infiltration Basin	Infiltration Basin	10700071	La Crosse - Sparta, Safety Rest Area No. 31	Basin A East	La Crosse	Southwest	IH	90	State		6/18/2024	5/1/2029		0.30	0	Monitor: sediment at IP1, riprap covered by dirt at IP1 and IP2, sediment buildup at IP3, erosion
418	Infiltration Basin	Infiltration Basin	10700071	La Crosse - Sparta, Safety Rest Area No. 31	Basin B	La Crosse	Southwest	IH	90	State		6/18/2024	5/1/2029		0.50	0	Clear out sediment and dense vegetation at IP1, monitor local depression at IP1 outflow area.
419	Bioswale	Biofilter	65360072	CTH K East - Sunnyside Road	Biofiltration device	Green Lake	North Central	CTH	A	Local							
420	Vegetated Rock Filter	Infiltration	37130270	Richmond Road, Walworth Avenue to Turtle Creek Bridge	Vegetated Rock Filter	Walworth	Southeast	CTH	P	Local							
421	Infiltration Basin	Wet Detention Pond with	59540273	CTH Q - STH 113 Road, (Willow Road - STH 113)	Infiltration Basin	Dane	Southwest	CTH	M	Local							

SCP ID	SCP TY	Final SCP Name	PROJ_ID	PROJ_DESC	SCP_DESC	CNTY	DOT_RGN	ROAD_SYS	ROAD_N M	Maintenance Responsibility	Permit Cycle 1 (2018-2023) Inspection Date	Permit Cycle 2 (2023-2028) Inspection Date	Next Planned SCP Insp. Date	PC1 Inspection Duration (hrs)	PC2 Inspection Duration (hrs)	Urgent Maint- enance	Inspection Notes / Maintenance-Repair Recommendation
422	Infiltration Basin	Infiltration Basin with Forebay	10660272	Madison - Milwaukee Road, (IH 90 - CTH N)	Infiltration Basin	Dane	Southwest	IH	94	State	8/1/2022		5/1/2027			0	Endwall #2 needs to be unclogged. Rip rap north of forebay needs to be unclogged
423	Infiltration Trench	Infiltration Trench	59890571	Inman Parkway Extension, City of Beloit (Prairie Avenue to Shopiere Road)	Infiltration Trench 1	Rock	Southwest	CTH	BT	Local							
424	Infiltration Trench	Infiltration Trench	59890571	Inman Parkway Extension, City of Beloit (Prairie Avenue to Shopiere Road)	Infiltration Trench 2	Rock	Southwest	CTH	BT	Local							
425	Infiltration Trench	Infiltration Trench	59890571	Inman Parkway Extension, City of Beloit (Prairie Avenue to Shopiere Road)	Infiltration Trench 3	Rock	Southwest	CTH	BT	Local							
426	Infiltration Trench	Infiltration Trench	59890571	Inman Parkway Extension, City of Beloit (Prairie Avenue to Shopiere Road)	Infiltration Trench 4	Rock	Southwest	CTH	BT	Local							
427	Infiltration Trench	Infiltration Trench	59890571	Inman Parkway Extension, City of Beloit (Prairie Avenue to Shopiere Road)	Infiltration Trench 5	Rock	Southwest	CTH	BT	Local							
428	Infiltration Trench	Infiltration Trench	59890571	Inman Parkway Extension, City of Beloit (Prairie Avenue to Shopiere Road)	Infiltration Trench 6	Rock	Southwest	CTH	BT	Local							
429	Infiltration Trench	Infiltration Trench	59890571	Inman Parkway Extension, City of Beloit (Prairie Avenue to Shopiere Road)	Infiltration Trench 7	Rock	Southwest	CTH	BT	Local							
430	Infiltration Trench	Infiltration Trench	59890571	Inman Parkway Extension, City of Beloit (Prairie Avenue to Shopiere Road)	Infiltration Trench 8	Rock	Southwest	CTH	BT	Local							
431	Infiltration Trench	Infiltration Trench	59890571	Inman Parkway Extension, City of Beloit (Prairie Avenue to Shopiere Road)	Infiltration Trench 9	Rock	Southwest	CTH	BT	Local							
432	Infiltration Trench	Infiltration Trench	59890571	Inman Parkway Extension, City of Beloit (Prairie Avenue to Shopiere Road)	Infiltration Trench 10	Rock	Southwest	CTH	BT	Local							
433	Infiltration Trench	Infiltration Trench	59890571	Inman Parkway Extension, City of Beloit (Prairie Avenue to Shopiere Road)	Infiltration Trench 11	Rock	Southwest	CTH	BT	Local							
434	Infiltration Trench	Infiltration Trench	59890571	Inman Parkway Extension, City of Beloit (Prairie Avenue to Shopiere Road)	Infiltration Trench 12	Rock	Southwest	CTH	BT	Local							
435	Infiltration Trench	Infiltration Trench	59890571	Inman Parkway Extension, City of Beloit (Prairie Avenue to Shopiere Road)	Infiltration Trench 13	Rock	Southwest	CTH	BT	Local							
436	Infiltration Trench	Infiltration Trench	59890571	Inman Parkway Extension, City of Beloit (Prairie Avenue to Shopiere Road)	Infiltration Trench 14	Rock	Southwest	CTH	BT	Local							
437	Dry Detention Basin	Dry Detention Pond	59890571	Inman Parkway Extension, City of Beloit (Prairie Avenue to Shopiere Road)	Dry Detention Basin 1	Rock	Southwest	CTH	BT	Local							
438	Dry Detention Basin	Dry Detention Pond	59890571	Inman Parkway Extension, City of Beloit (Prairie Avenue to Shopiere Road)	Dry Detention Basin 2	Rock	Southwest	CTH	BT	Local							
439	Wet Detention Pond	Wet Detention	86800071	City of Superior, Belknap Street, Banks Avenue to Hill Avenue	Hill Avenue Storm Water Basin	Douglas	Northwest	USH	2	Connecting Hwy				0.00			
440	Dry Pond	Dry Detention	11804371	Superior - Brule, USH 53 Interchange to CTH D	Stormwater Pond 1	Douglas	Northwest	USH	2	State		6/24/2025	5/1/2025	0.00			
441	Dry Pond	Dry Detention	11804371	Superior - Brule, USH 53 Interchange to CTH D	Stormwater Pond 2	Douglas	Northwest	USH	2	State		6/24/2025	5/1/2025	0.00			
442	Dry Pond	Dry Detention	11804371	Superior - Brule, USH 53 Interchange to CTH D	Stormwater Pond 3	Douglas	Northwest	USH	2	State		6/24/2025	5/1/2025	0.00			
443	Wet Detention Pond	Wet Detention	21601070	South 76th Street, 600' N of High Street to Carter Boulevard	Retention Basin	Milwaukee	Southeast	CTH	U	Local							
444	Drainage Swale	Grassed Swale	69991370	Camp Phillips Rd (CTH X), Weston, Bernard Ave - Northwestern Ave	Drainage Swale	Marathon	North Central	Local Road	Camp Phillips Rd	Local							
446	Dry Detention Pond	Dry Detention Pond	10302072	N-S Freeway - College Av to Howard Av, Mitchell Interchange	Stormwater Detention Basin North Lobe	Milwaukee	Southeast	IH	43	State	7/19/2023		5/1/2028	0.75		0	Endwall #2 covered with dense vegetation which should be monitored.
447	Dry Detention Pond	Dry Detention Pond	10302072	N-S Freeway - College Av to Howard Av, Mitchell Interchange	Stormwater Detention Basin South Lobe	Milwaukee	Southeast	IH	43	State	7/19/2023		5/1/2028	0.50		0	Cattails surrounding Outlet Devices should be controlled to locate grate and assess condition. Endwall #3 should be monitored for vegetation accumulation
448	Dry Detention Pond	Dry Detention	81402271	City of Hayward, Anderson Road - CTH B	Detention Basin	Sawyer	Northwest	STH	27	State		6/24/2025	5/1/2025	0.00			
449	Wet Detention Pond	Wet Detention Pond	69990881	USH 51/STH 29 Corridor - Wausau, Stewart Avenue, City of Wausau	Detention Basin	Marathon	North Central	Local Road	Stewart Avenue	Local							
450	Dry Pond	Dry Detention	12060184	Madison - Cambridge, Fish Hatchery Road Interchange	Dry Pond	Dane	Southwest	USH	12	State	8/1/2022		5/1/2027			0	Endwalls #1 and #5 are missing pipe markers that should be replaced
451	Dry Detention Basin	Dry Detention	13900481	Watertown - STH 60 East Road, CTH Q to CTH J	Dry Detention Basin	Dodge	Southwest	STH	26	State		5/15/2024	5/1/2029		1.00	0	Monitor standing water and displacement of riprap at IP1. Monitor rilling near IP8 and IP9
452	Dry Pond	Dry Detention	20401670	Ryan Road (STH 100), City Oak Creek, 27th Street - STH 38	Dry Pond	Milwaukee	Southeast	STH	100	State	6/20/2023		5/1/2028	0.25		0	
453	Proprietary Device - Baysaver	Proprietary Device	23650670	W. Grange Ave. and S. 60th St, 76th - Loomis and 1500' North and South of W. Grange Ave.	Stormwater Treatment Device No. 1	Milwaukee	Southeast	Local Road	W. Grange Avenue	Local							
454	Proprietary Device - Baysaver	Proprietary Device	23650670	W. Grange Ave. and S. 60th St, 76th - Loomis and 1500' North and South of W. Grange Ave.	Stormwater Treatment Device No. 2	Milwaukee	Southeast	Local Road	W. Grange Avenue	Local							
455	Proprietary Device - Stormceptor	Proprietary Device	23650670	W. Grange Ave. and S. 60th St, 76th - Loomis and 1500' North and South of W. Grange Ave.	Stormceptor 1	Milwaukee	Southeast	Local Road	W. Grange Avenue	Local							
456	Proprietary Device - Stormceptor	Proprietary Device	23650670	W. Grange Ave. and S. 60th St, 76th - Loomis and 1500' North and South of W. Grange Ave.	Stormceptor 2	Milwaukee	Southeast	Local Road	W. Grange Avenue	Local							
457	Proprietary Device - Baysaver	Proprietary Device	61970173	Main Street Bridge and Approaches, Village of DeForest, (Yahara River Bridge)	Baysaver 5K	Dane	Southwest	CTH	CV	Local							
458	Proprietary Device - Baysaver	Proprietary Device	61970173	Main Street Bridge and Approaches, Village of DeForest, (Yahara River Bridge)	Baysaver 3K, modified	Dane	Southwest	CTH	CV	Local							
459	Proprietary Device - Baysaver	Proprietary Device	36750071	Village of Rockdale, CTH B, (Koshkonong Creek Bridge B-13-612)	Baysaver 3K	Dane	Southwest	CTH	B	Local							
460	Proprietary Device - Baysaver	Proprietary Device	36750071	Village of Rockdale, CTH B, (Koshkonong Creek Bridge B-13-612)	Baysaver 10K	Dane	Southwest	CTH	B	Local							
461	Proprietary Device - Baysaver	Proprietary Device	53000379	Sauk City to Middleton Road, (Springton Drive-Parmenter Street)	Baysaver 10K	Dane	Southwest	USH	12	Local							
462	Proprietary Device - Stormceptor	Proprietary Device	23501070	Racine Street, City of Racine, 21st Street to Washington Avenue	STC 1800	Racine	Southeast	STH	32	Connecting Hwy				0.00			
463	Proprietary Device - Stormceptor	Proprietary Device	37130270	Richmond Road, Walworth Avenue to Turtle Creek Bridge	Stormceptor 1200	Walworth	Southeast	CTH	P	Local							
464	Proprietary Device - Stormceptor	Proprietary Device	20460270	W. Burnham Street, 400' E of S 52nd to Miller Park Way	Storm Water Sedimentation device (STC 7200)	Milwaukee	Southeast	Local Road	W. Burnham Street	Local							
465	Proprietary Device - Vortechs	Proprietary Device	46100671	Village of Sister Bay, Country Walk Drive - Scandia Road	Stormwater treatment device 1 (Vortechs 7000)	Door	Northeast	STH	42	State		11/8/2024	5/1/2029		0.33	0	Remove Trash/Debris/Sediment for MH 2,3. Monitor water and sediment level at MH 1,2.
466	Proprietary Device - Vortechs	Proprietary Device	46100671	Village of Sister Bay, Country Walk Drive - Scandia Road	Stormwater treatment device 2 (Vortechs 7000)	Door	Northeast	STH	42	State		11/8/2024	5/1/2029		0.33	0	Monitor Debris accumulation at MH 3

SCP_ID	SCP_TY	Final SCP Name	PROJ_ID	PROJ_DESC	SCP_DESC	CNTY	DOT_RGN	ROAD_SYS	ROAD_N M	Maintenance Responsibility	Permit Cycle 1 (2018-2023) Inspection Date	Permit Cycle 2 (2023-2028) Inspection Date	Next Planned SCP Insp. Date	PC1 Inspection Duration (hrs)	PC2 Inspection Duration (hrs)	Urgent Maint- enance	Inspection Notes / Maintenance-Repair Recommendation
467	Permanent Sediment Basin	Wet Detention	91950770	Phelp - CTH A, St. Louis Rd - CTH A	Millerville Sediment Basin	Vilas	North Central	STH	17	State			5/1/2025	0.00			
468	Wet Detention Pond	Wet Detention Pond	69960570	City of Portage, E Haertel Street, New Pinery Road to E Albert Street	Sedimentation Basin	Columbia	Southwest	Local Road	E Haertel Street	Local							
469	Filter Strip	Filter Strip	69960570	City of Portage, E Haertel Street, New Pinery Road to E Albert Street	Filter Strip 1	Columbia	Southwest	Local Road	E Haertel Street	Local							
470	Filter Strip	Filter Strip	69960570	City of Portage, E Haertel Street, New Pinery Road to E Albert Street	Filter Strip 2	Columbia	Southwest	Local Road	E Haertel Street	Local							
471	Filter Strip	Filter Strip	69960570	City of Portage, E Haertel Street, New Pinery Road to E Albert Street	Filter Strip 3	Columbia	Southwest	Local Road	E Haertel Street	Local							
472	Filter Strip	Filter Strip	69960570	City of Portage, E Haertel Street, New Pinery Road to E Albert Street	Filter Strip 4	Columbia	Southwest	Local Road	E Haertel Street	Local							
473	Filter Strip	Filter Strip	10010374	City of Janesville, STH 11 N Jct Interchange	Ramp RB Filter Strip	Rock	Southwest	IH	39	State	5/6/2024	5/1/2029		0.05	0	Monitor erosion and litter along the bottom of the ditch	
474	Filter Strip	Filter Strip	10010374	City of Janesville, STH 11 N Jct Interchange	Ramp RC Filter Strip	Rock	Southwest	IH	39	State	5/6/2024	5/1/2029		0.10	0	No Comments	
475	Filter Strip	Filter Strip	10010374	City of Janesville, STH 11 N Jct Interchange	Ramp RA Filter Strip	Rock	Southwest	IH	39	State	5/6/2024	5/1/2029		0.20	0	No Comments	
476	Infiltration Basin	Infiltration Basin	10010374	City of Janesville, STH 11 N Jct Interchange	NW-W Infiltration Basin	Rock	Southwest	IH	39	State	5/6/2024	5/1/2029		0.50	0	Remove sandbags obstructing endwall at outlet 1	
477	Infiltration Basin	Infiltration Basin	10010374	City of Janesville, STH 11 N Jct Interchange	NW Infiltration Basin	Rock	Southwest	IH	39	State	5/6/2024	5/1/2029		0.25	0	Monitor bare soil patches and minor rilling	
478	Infiltration Basin	Infiltration Basin	10010374	City of Janesville, STH 11 N Jct Interchange	NE Infiltration Basin	Rock	Southwest	IH	39	State	5/6/2024	5/1/2029		1.00	0	Monitor erosion and loss of dam material at southeast corner	
479	Infiltration Basin	Infiltration Basin	10010374	City of Janesville, STH 11 N Jct Interchange	SE Infiltration Basin	Rock	Southwest	IH	39	State	5/6/2024	5/1/2029		1.00	1	2024 Urgent Maintenance: Endwall was noted that it should self-clean. Plans to remove trees/brush in the	
480	Infiltration Basin	Infiltration Basin	10010374	City of Janesville, STH 11 N Jct Interchange	SW Infiltration Basin	Rock	Southwest	IH	39	State	5/6/2024	5/1/2029		1.00	1	2024 Urgent Maintenance: Embankment seepage was unable to be located. Will remove trees in	
481	Filter Strip	Enhanced Filter Strip	10031071	Illinois State Line - Madison, CTH S, Shopiere Road Interchange	Filter Strip 1	Rock	Southwest	IH	39	State	6/1/2021		5/1/2026				
482	Filter Strip	Enhanced Filter Strip	10031071	Illinois State Line - Madison, CTH S, Shopiere Road Interchange	Filter Strip 2	Rock	Southwest	IH	39	State	6/1/2021		5/1/2026				
483	Filter Strip	Enhanced Filter Strip	10031071	Illinois State Line - Madison, CTH S, Shopiere Road Interchange	Filter Strip 3	Rock	Southwest	IH	39	State	6/1/2021		5/1/2026				
484	Filter Strip	Enhanced Filter Strip	10031071	Illinois State Line - Madison, CTH S, Shopiere Road Interchange	Filter Strip 4	Rock	Southwest	IH	39	State	6/1/2021		5/1/2026				
485	Filter Strip	Enhanced Filter Strip	10031071	Illinois State Line - Madison, CTH S, Shopiere Road Interchange	Filter Strip 5	Rock	Southwest	IH	39	State	6/1/2021		5/1/2026				
487	Filter Strip	Enhanced Filter Strip	10031071	Illinois State Line - Madison, CTH S, Shopiere Road Interchange	Filter Strip 7	Rock	Southwest	IH	39	State	6/1/2021		5/1/2026				
488	Stormwater Quality Vault	Concrete Vault	11951371	City of Superior, Tower Avenue, Belknap Street to 3rd Street	Grit Chamber Type A 1	Douglas	Northwest	STH	35	Connecting Hwy				0.00			
489	Stormwater Quality Vault	Concrete Vault	11951371	City of Superior, Tower Avenue, Belknap Street to 3rd Street	Grit Chamber Type A 2	Douglas	Northwest	STH	35	Connecting Hwy				0.00			
490	Stormwater Quality Vault	Concrete Vault	11951371	City of Superior, Tower Avenue, Belknap Street to 3rd Street	Grit Chamber Type A 3	Douglas	Northwest	STH	35	Connecting Hwy				0.00			
491	Stormwater Quality Vault	Concrete Vault	11951371	City of Superior, Tower Avenue, Belknap Street to 3rd Street	Grit Chamber Type A 4	Douglas	Northwest	STH	35	Connecting Hwy				0.00			
492	Stormwater Quality Vault	Concrete Vault	11951371	City of Superior, Tower Avenue, Belknap Street to 3rd Street	Grit Chamber Type A 5	Douglas	Northwest	STH	35	Connecting Hwy				0.00			
493	Stormwater Quality Vault	Concrete Vault	11951371	City of Superior, Tower Avenue, Belknap Street to 3rd Street	Grit Chamber Tybe B 1	Douglas	Northwest	STH	35	Connecting Hwy				0.00			
494	Stormwater Quality Vault	Concrete Vault	89982472	North 28th Street, City of Superior, (Superior School District Parking Lots)	Grit Chamber	Douglas	Northwest	Local Road	North 28th Street	Local							
495	Dry Detention Pond	Dry Detention	60200271	Madison - Deforest Road, Reardon Road - Vinburn Road	Pond P1	Dane	Southwest	USH	51	State	8/1/2022		5/1/2027			1	Endwall #4 needs urgent maintenance. Undercutting has progressed to the point of structural deficiency
496	Dry Detention Pond	Dry Detention	60200271	Madison - Deforest Road, Reardon Road - Vinburn Road	Pond P2	Dane	Southwest	USH	51	State	8/1/2022		5/1/2027			0	Some undercutting beneath endwall #3 needs to be repaired
497	Dry Detention Pond	Dry Detention	60200271	Madison - Deforest Road, Reardon Road - Vinburn Road	Pond P3	Dane	Southwest	USH	51	State	8/1/2022		5/1/2027			0	
498	Dry Detention Pond	Dry Detention	60200271	Madison - Deforest Road, Reardon Road - Vinburn Road	Pond P4	Dane	Southwest	USH	51	State	8/1/2022		5/1/2027			0	
499	Dry Detention Pond	Dry Detention	60200271	Madison - Deforest Road, Reardon Road - Vinburn Road	Pond P5	Dane	Southwest	USH	51	State	8/1/2022		5/1/2027			0	
500	Dry Detention Pond	Dry Detention	10603170	East-West Freeway, CTH P Interchange	South Pond	Waukesha	Southeast	IH	94	State	7/19/2023		5/1/2028	0.80		0	Endwalls 9 and 10 and Curb Inlets 1,2, and 3 were not in the plans and were most likely altered after the
501	Dry Detention Pond	Dry Detention	10603170	East-West Freeway, CTH P Interchange	Middle Pond	Waukesha	Southeast	IH	94	State	7/5/2023		5/1/2028	0.50		0	Riprap is placed 2ft to the left of endwall #6
502	Dry Detention Pond	Dry Detention	10603170	East-West Freeway, CTH P Interchange	North Pond	Waukesha	Southeast	IH	94	State	7/5/2023		5/1/2028	0.40		0	Joints connecting apron endwalls are disconnected from Endwall #1. Large population of cattails in basin.
503	Infiltration Pond	Dry Detention Pond with	11661274	Stevens Point - Wausau, Bus 51 Interchange	North Dry Pond	Marathon	North Central	IH	39	State	8/16/2022		5/1/2027			0	Sediment and Vegetation should be removed from end wall #1
504	Infiltration Pond	Dry Detention Pond with	11661274	Stevens Point - Wausau, Bus 51 Interchange	South Dry Pond	Marathon	North Central	IH	39	State	8/16/2022		5/1/2027			0	
505	Stormwater Quality Vault	Concrete Vault	13001370	Hoan Bridge & Lake Freeway Milwaukee River to Carferry Dr Bridge Rehabilitation	Stormwater quality vault	Milwaukee	Southeast	IH	794	State	7/19/2023	11/8/2024	11/8/2024	0.50	0.33	0	No Comments. MH2 not found. Under apparent shrubs. One manhole was accessible.
506	Vegetated Rock Filter	Infiltration	31600170	Walworth to Elkhorn Road, S. Main Street to Theatre Road	Vegetated Rock Filter	Walworth	Southeast	STH	67	State		6/4/2024	5/1/2029		0.25	0	No Comments
507	Outlet Pipe Sediment Trap	Stilling Basin	11200983	STH 26 - Breezewood Lane, USH 45 - Breezewood Lane, USH 41 Mainline Widening and Breezewood Lane Interchange	Outlet Pipe Sediment Trap 1	Winnebago	Northeast	USH	41	State	6/16/2022		5/1/2027			0	Remove sediment and vegetation within trap and refresh stone.
508	Outlet Pipe Sediment Trap	Stilling Basin	11200983	STH 26 - Breezewood Lane, USH 45 - Breezewood Lane, USH 41 Mainline Widening and Breezewood Lane Interchange	Outlet Pipe Sediment Trap 2	Winnebago	Northeast	USH	41	State		7/18/2024	5/1/2029		0.08	0	Monitor cattail coverage in stilling basin area.
509	Outlet Pipe Sediment Trap	Stilling Basin	11200983	STH 26 - Breezewood Lane, USH 45 - Breezewood Lane, USH 41 Mainline Widening and Breezewood Lane Interchange	Outlet Pipe Sediment Trap 3	Winnebago	Northeast	USH	41	State	6/16/2022		5/1/2027			0	Remove sediment and vegetation within trap and refresh stone.
510	Outlet Pipe Sediment Trap	Stilling Basin	11200971	STH 26 - Breezewood Lane, USH 45 - Breezewood Lane, USH 41 Mainline	Outlet Pipe Sediment Trap 1	Winnebago	Northeast	USH	41	State		7/18/2024	5/1/2029		0.08	0	Remove sediment accumulation from IP1 and stilling basin bottom. Monitor cattail growth (100%).
511	Outlet Pipe Sediment Trap	Stilling Basin	11200971	STH 26 - Breezewood Lane, USH 45 - Breezewood Lane, USH 41 Mainline	Outlet Pipe Sediment Trap 2	Winnebago	Northeast	USH	41	State		7/18/2024	5/1/2029		0.10	1	2024 Urgent Maintenance: Remove sediment and debris at IP1. Remove sediment from stilling basin bottom. Monitor 100% cattail growth.
512	Outlet Pipe Sediment Trap	Stilling Basin	11200971	STH 26 - Breezewood Lane, USH 45 - Breezewood Lane, USH 41 Mainline	Outlet Pipe Sediment Trap 3	Winnebago	Northeast	USH	41	State		7/18/2024	5/1/2029		0.08	1	2024 Urgent Maintenance: Remove sediment and debris at IP1. Remove sediment from stilling basin bottom. Monitor 40% cattail growth.
513	Outlet Pipe Sediment Trap	Stilling Basin	11200971	STH 26 - Breezewood Lane, USH 45 - Breezewood Lane, USH 41 Mainline	Outlet Pipe Sediment Trap 4	Winnebago	Northeast	USH	41	State		7/18/2024	5/1/2029		0.08	0	Remove sediment in IP1. Monitor sediment accumulation and cattail growth in main basin.
514	Outlet Pipe Sediment Trap	Stilling Basin	11200971	STH 26 - Breezewood Lane, USH 45 - Breezewood Lane, USH 41 Mainline	Outlet Pipe Sediment Trap 5	Winnebago	Northeast	USH	41	State		7/18/2024	5/1/2029		0.08	0	Remove sediment accumulation from IP1. Monitor: sediment accumulation on bottom, 100% cattail growth.
515	Outlet Pipe Sediment Trap	Stilling Basin	11200971	STH 26 - Breezewood Lane, USH 45 - Breezewood Lane, USH 41 Mainline	Outlet Pipe Sediment Trap 6	Winnebago	Northeast	USH	41	State		7/18/2024	5/1/2029		0.08	0	Monitor grassy vegetation in endwall of IP1.
516	Outlet Pipe Sediment Trap	Stilling Basin	11200971	STH 26 - Breezewood Lane, USH 45 - Breezewood Lane, USH 41 Mainline	Outlet Pipe Sediment Trap 7	Winnebago	Northeast	USH	41	State		7/18/2024	5/1/2029		0.08	0	Remove vegetation and debris at IP1.
517	Outlet Pipe Sediment Trap	Stilling Basin	11200971	STH 26 - Breezewood Lane, USH 45 - Breezewood Lane, USH 41 Mainline	Outlet Pipe Sediment Trap 8	Winnebago	Northeast	USH	41	State		7/18/2024	5/1/2029		0.08	0	Remove sediment I IP1. Monitor dense cattail coverage in main basin area.
518	Outlet Pipe Sediment Trap	Stilling Basin	11200971	STH 26 - Breezewood Lane, USH 45 - Breezewood Lane, USH 41 Mainline	Outlet Pipe Sediment Trap 9	Winnebago	Northeast	USH	41	State		6/27/2024	5/1/2029		0.10	0	Remove dense cattails at IP1, monitor 4" of standing water in stilling basin.
519	Outlet Pipe Sediment Trap	Stilling Basin	11200971	STH 26 - Breezewood Lane, USH 45 - Breezewood Lane, USH 41 Mainline	Outlet Pipe Sediment Trap 10	Winnebago	Northeast	USH	41	State		6/27/2024	5/1/2029		0.10	0	Remove cattails at IP1, monitor standing water in stilling basin.

SCP ID	SCP TY	Final SCP Name	PROJ_ID	PROJ_DESC	SCP_DESC	CNTY	DOT_RGN	ROAD_SYS	ROAD_N M	Maintenance Responsibility	Permit Cycle 1 (2018-2023) Inspection Date	Permit Cycle 2 (2023-2028) Inspection Date	Next Planned SCP Insp Date	PC1 Inspection Duration (hrs)	PC2 Inspection Duration (hrs)	Urgent Maint- enance	Inspection Notes / Maintenance-Repair Recommendation
641	Dry Detention Pond	Dry Detention	16740080	Lake Delton - Sauk City Road, Terrytown Rd to Ski Hi Rd, Phase 1	CTH W Pond	Sauk	Southwest	USH	12	State	7/25/2022		5/1/2027			0	The inflow points need rip rap to prevent scour and undercutting
642	Swale	Grass Swale	16740080	Lake Delton - Sauk City Road, Terrytown Rd to Ski Hi Rd, Phase 1	Drainage Swale	Sauk	Southwest	USH	12	State	7/25/2022		5/1/2027			0	
643	Wet Detention Pond	Wet Detention	10650472	Madison to Milwaukee Road, CTH SS Interchange	Detention Pond	Waukesha	Southeast	IH	94	State	7/19/2023		5/1/2028	1.00		0	Endwall #1,2 and #4 need to be unlogged. Outlet structure covered in trees.
644	Wet Detention Pond	Wet Detention	10651070	Madison to Milwaukee Road, CTH G Interchange	Detention Pond 1	Waukesha	Southeast	IH	94	State	8/8/2023		5/1/2028	0.50		0	
645	Wet Detention Pond	Wet Detention	10651070	Madison to Milwaukee Road, CTH G Interchange	Detention Pond 2	Waukesha	Southeast	IH	94	State	8/8/2023		5/1/2028	0.50		0	
646	Rain Garden	Infiltration Basin	52810171	Corner Street, City of Lodi, South Corporate Limits - Sodders Street	Rain Garden	Columbia	Southwest	STH	113	State	7/25/2022		5/1/2027			0	Trees growing in Cell C weir need to be removed. End section of Underdrain #2 should be repaired. Hole in
647	Dry Detention Pond	Dry Detention	16470971	Richland Center - Gotham, USH 14	Detention Pond	Richland	Southwest	USH	14	State		5/22/2024	5/1/2029		0.50	1	2024 Urgent Maintenance: 2. End section of Underdrain #2 should be repaired.
648	Wet Detention Pond	Wet Detention	21400672	Wauwatosa Road, Freistadt Road and Highland Road Intersections	Detention Pond	Ozaukee	Southeast	STH	181	Connecting Hwy				0.00		1	3. Hole in asphalt path should be filled
649	Dry Detention Pond	Dry Detention	16610971	Prairie du Chien - Bridgeport, La Pointe Street - South Town Lane	Detention Basin	Crawford	Southwest	USH	18	State	6/1/2022	5/22/2024	5/1/2029		0.30	0	Outlet pipe may need repair. The damage does not seem to impact the integrity of pipe, and therefore does not pose risk to the roadway. Inflow pipe requires a marker indicating its location.
650	Dry Detention Pond	Dry Detention	16610971	Prairie du Chien - Bridgeport, La Pointe Street - South Town Lane	Retention Pond	Crawford	Southwest	USH	18	State	6/1/2022		5/1/2029		0.30	0	No Comments
651	Wet Detention Pond	Wet Detention	27480170	Lovers Lane Rd, STH 175 - STH 60	Retention Pond 1	Washington	Southeast	STH	164	State		8/6/2024	5/1/2029		0.50	0	Repair:Trees and Woody Vegetation at IP 1, Severe undercutting and eroded at IP 1, Washed out riprap at IP
652	Dry Detention Pond	Pond	27480170	Lovers Lane Rd, STH 175 - STH 60	Retention Pond 2	Washington	Southeast	STH	164	State		8/6/2024	5/1/2029		0.48	0	Repair: Grassy vegetation and standing water at IP 1.
653	Wet Detention Pond	Wet Detention	17040172	IH 90 - STH 89 Road, CTH MM - CTH O	Detention Basin	Rock	Southwest	USH	14	State		5/30/2024	5/1/2029		0.25	0	Monitor dense cattail in pond outlet device and at inflow point 2
654	Dry Detention Pond	Dry Detention	10110174	Lake Delton - Madison Road, USH 151 Interchange	Detention Pond	Dane	Southwest	IH	90	State	5/23/2022		5/1/2027			1	Needs work on deteriorating inlet pipe (endwall #5) and other clogged inlets (endwalls #1,2,3,8, and 13)
655	Dry Detention Pond	Dry Detention	13200970	Sylvania Park & Ride Lot, I-94 / STH 11 Interchange	Detention Pond	Racine	Southeast	STH	11	State	6/12/2023		5/1/2028	0.50		0	
656	Wet Detention Pond	Wet Detention	27810170	Chinook Pass, IH 43 - STH 59	Water Quality Pond	Waukesha	Southeast	STH	164	State		8/2/2024	5/1/2029		0.70	0	Monitor: vegetation in IP1, erosion in forebay
657	Wet Detention Pond	Wet Detention	10650970	Barker Rd, Bridge Widening	Detention Basin	Waukesha	Southeast	IH	94	State	8/1/2023		5/1/2028	0.75		0	Endwall #3 & #4 are clogged with sediment and vegetation. Endwall #7 could not be located, but the riprap
658	Wet Detention Pond	Wet Detention	40100470	North Main Street, STH 33 - Barton Avenue	Barton Hill Pond	Washington	Southeast	STH	144	Connecting Hwy				0.00		0	
659	Wet Detention Pond	Wet Detention	13910571	Jefferson - Watertown Road, CTH Y - Baneck Road	Detention Basin	Jefferson	Southwest	STH	26	State		5/6/2024	5/1/2029		0.60	0	Work needed on channelized erosion (IP1 and IP3), separated endwalls (IP1 and IP3), trees growing near endwalls (IP1 and IP2), trees impacting NW and W embankments to be removed, trees impacting outlet
660	Wet Detention Pond	Wet Detention	13710772	Oconomowoc Bypass, 1/2 Mile West of STH 67 to STH 16, Grading and Base	South Detention Pond	Waukesha	Southeast	STH	16	State	6/1/2023		5/1/2028	0.50		0	Endwall #1 is ~25% clogged. This should be cleared from sediment and vegetation
661	Wet Detention Pond	Wet Detention	13710772	Oconomowoc Bypass, 1/2 Mile West of STH 67 to STH 16, Grading and Base	North Detention Pond	Waukesha	Southeast	STH	16	State	6/1/2023		5/1/2028	0.50		0	Monitor trees near Endwall #1 as their downstream roots may damage the inlet pipe
662	Swale	Grass Swale	13710772	Oconomowoc Bypass, 1/2 Mile West of STH 67 to STH 16, Grading and Base	Infiltration Area C	Waukesha	Southeast	STH	16	State	6/1/2023		5/1/2028	0.50		0	Clear the vegetation from the pipe at Endwall #3 . There were cattails present (~65%) in the infiltration area.
663	Infiltration Basin	Infiltration Basin	13710772	Oconomowoc Bypass, 1/2 Mile West of STH 67 to STH 16, Grading and Base	Infiltration Area B	Waukesha	Southeast	STH	16	State	6/1/2023		5/1/2028	0.50		0	The location of the infiltration basin was not in the location shown per the plans. The infiltration basin is not infiltrating water, effectively acting as a wet pond
664	Infiltration Basin	Infiltration Basin	13710772	Oconomowoc Bypass, 1/2 Mile West of STH 67 to STH 16, Grading and Base	Infiltration Area A	Waukesha	Southeast	STH	16	State	6/1/2023		5/1/2028	0.50		0	50% cattails present, indicates standing water in the infiltration area
665	Bioretention Basin	Biofilter	13710772	Oconomowoc Bypass, 1/2 Mile West of STH 67 to STH 16, Grading and Base	Bioretention Pond	Waukesha	Southeast	STH	16	State	6/1/2023		5/1/2028	0.50		0	Basin area is acting like a wet detention pond rather than a bioretention pond
666	Wet Detention Pond	Wet Detention	11900087	City of Altoona, Tenth Street, Spooner Avenue - East Willson Drive, USH 53 / Eau Claire County	Pond	Eau Claire	Northwest	USH	53	Local				0.00		0	
667	Infiltration Basin	Infiltration Basin	11900086	Eau Claire - Chippewa Falls Road, USH 12 Interchange: B-18-173 & B-18-174	Permanent Sediment Basin	Eau Claire	Northwest	USH	53	State		8/13/2024	5/1/2029		0.42	0	Monitor: undermining at IP1, joint separation at IP2, erosion channel in main basin
668	Dry Detention Pond	Dry Detention	13310872	Mukwonago Bypass, STH 83 to Holz Drive	Detention Pond A	Waukesha	Southeast	Local Road		Bay View Road	Local						
669	Dry Detention Pond	Dry Detention	13310872	Mukwonago Bypass, STH 83 to Holz Drive	Detention Pond B	Waukesha	Southeast	Local Road		Bay View Road	Local						
670	Wet Detention Pond	Wet Detention	13310872	Mukwonago Bypass, STH 83 to Holz Drive	Detention Pond C	Waukesha	Southeast	Local Road		Bay View Road	Local						
671	Wet Detention Pond	Wet Detention	13310872	Mukwonago Bypass, STH 83 to Holz Drive	Detention Pond D	Waukesha	Southeast	Local Road		Bay View Road	Local						
672	Wet Detention Pond	Wet Detention	13310870	Rochester Street, Wolf Run to School Rd (CTH NN)	Detention Pond E	Waukesha	Southeast	STH	83	State	8/28/2023		5/1/2028	0.50		0	WN: Cattails in main pool (8-10' tall and invasive), erosion/undercutting at endwall #2, access to overall site;
673	Wet Detention Pond	Wet Detention	13310870	Rochester Street, Wolf Run to School Rd (CTH NN)	Detention Pond F	Waukesha	Southeast	STH	83	State	8/28/2023		5/1/2028	0.50		2	M: Obstruction at Endwall #1 & basin overflow, erosion/undercutting at endwalls #3 & #4
674	Wet Detention Pond	Wet Detention	13310870	Rochester Street, Wolf Run to School Rd (CTH NN)	Detention Pond G	Waukesha	Southeast	STH	83	State	8/28/2023		5/1/2028	0.50		0	Urgent: Endwall #2 is fully clogged and outlet device severely damaged. WN: Endwall 1 & 2 obstructed,
675	Wet Detention Pond	Wet Detention	13310870	Rochester Street, Wolf Run to School Rd (CTH NN)	Detention Pond H	Waukesha	Southeast	STH	83	State	8/8/2023		5/1/2028	0.33		0	WN: Obstruction of inflow Endwall #1, outlet Endwall #2, #3, & #4. M: Obstructed overflow weir, 80%
676	Wet Detention Pond	Wet Detention	13310870	Rochester Street, Wolf Run to School Rd (CTH NN)	Detention Pond I	Waukesha	Southeast	STH	83	State	8/8/2023		5/1/2028	0.33		0	M: Sediment, standing water, & no riprap observed at Endwall #1; Cattails present around safety shelf
677	Infiltration Trench	Trench	12060780	Mt Horeb - Madison, Military Ridge Park & Ride Expansion	Stone Weeper Trench	Dane	Southwest	USH	18	State	6/15/2022		5/1/2027			0	Vegetation growing at toe of endwall is impeding proper drainage.
678	Wet Detention Pond	Wet Detention	59910916	Briggs Road - CTH HD, (Briggs Road - North Star Rd)	Pond 1	La Crosse	Southwest	CTH	MH	Local							
679	Wet Detention Pond	Wet Detention	59910916	Briggs Road - CTH HD, (Briggs Road - North Star Rd)	Pond 2	La Crosse	Southwest	CTH	MH	Local							
680	Forebay	Infiltration Basin	11660570	USH 51/STH 29 Corridor - Wausau, STH 29 East System Ramps, Fox Glove Road to Lily Lane	Storm Forebay	Marathon	North Central	STH	29	State	8/15/2022		5/1/2027			0	
681	Dry Detention Pond	Dry Detention	31801270	Burlington Bypass, STH 36 and CTH P Interchanges	Detention Pond	Racine	Southeast	STH	11	State	7/17/2023		5/1/2028	0.75		0	Monitor Endwall #7 for sediment accumulation
682	Wet Detention Pond	Wet Detention	11030772	USH 41/45 and STH 100/145 (North I/C), STH 145 Reconstruction	Detention Pond	Milwaukee	Southeast	STH	145	State	8/8/2023		5/1/2028	0.75		0	The pond's water level is higher than it is shown in the plan. It is assumed that Endwall #6 is completely
683	Infiltration Ditch	Enhanced Grass Swale	13900472	Jefferson Bypass, STH 89 - USH 18	Special Infiltration Ditch	Jefferson	Southwest	STH	26	State		6/20/2025	5/1/2025	0.00			
684	Infiltration Ditch	Enhanced Grass Swale	13900474	Jefferson Bypass, STH 89 - Jahn Road	Special Infiltration Ditch	Jefferson	Southwest	STH	26	State		5/15/2024	5/1/2029		0.20	0	Monitor heav vegetation at end wall and minor erosion. Monitor small soft spot on embankment.
685	Dry Detention Pond	Dry Detention	13900486	Janesville to Fort Atkinson, CTY Y - Town Line Road	Storm Sewer Pond	Rock	Southwest	STH	26	State		5/30/2024	5/1/2029		0.30	0	Monitor minor underrcutting at inflow points 1 and 2
686	Wet Detention Pond	Pond	10350372	N-S Freeway - CTH KR Interchange, Mainline and Ramps	Detention Pond	Kenosha	Southeast	IH	94	State	8/10/2023		5/1/2028	0.75		0	Endwalls #2, #3, and #7 should be cleared of vegetation to facilitate easy access to these endwalls.
687	Wet Detention Pond	Wet Detention	10302372	N-S Freeway - CTH E Interchange,, Mainline and Ramps	NW Detention Pond	Kenosha	Southeast	IH	94	State	6/26/2023		5/1/2028	0.75		0	
688	Dry Pond	Dry Detention	10302472	N-S Freeway - STH 11 Interchange,, Mainline and Ramps	SW Infield Pond	Racine	Southeast	IH	94	State	6/12/2023		5/1/2028	0.50		0	
689	Dry Pond	Dry Detention	10302472	N-S Freeway - STH 11 Interchange,, Mainline and Ramps	SE Infield Pond	Racine	Southeast	IH	94	State	6/12/2023		5/1/2028	0.50		0	
690	Dry Pond	Dry Detention	10302472	N-S Freeway - STH 11 Interchange,, Mainline and Ramps	NE Pond	Racine	Southeast	IH	94	State	6/12/2023		5/1/2028	0.50		0	
691	Wet Detention Pond	Wet Detention	13800070	Mequon Road, Village of Germantown,, Country Aire Dr Intersection	Wet Detention Pond	Washington	Southeast	STH	167	State		8/6/2024	5/1/2029		0.58	0	Sediment and vegetation at IP 1 and Cattails at IP 3
692	Drainage Swale	Grassed Swale	13800070	Mequon Road, Village of Germantown,, Country Aire Dr Intersection	Drainage Swale	Washington	Southeast	STH	167	State		6/20/2025	5/1/2025	0.00			
693	Wet Detention Pond	Wet Detention	32400572	Sheridan Road, 16th Place to CTH KR	Detention Pond	Kenosha	Southeast	STH	32	State		8/14/2024	5/1/2029		0.75	0	Monitor dense woody vegetation at embankment and Outlet device 1
694	Infiltration Basin	Infiltration Basin	23550575	East College Avenue, S Howell Ave - S Pennsylvania Ave	Infiltration basin	Milwaukee	Southeast	CTH	ZZ	Local							

SCP ID	SCP TY	Final SCP Name	PROJ_ID	PROJ_DESC	SCP_DESC	CNTY	DOT_RGN	ROAD_SYS	ROAD_N M	Maintenance Responsibility	Permit Cycle 1 (2018-2023) Inspection Date	Permit Cycle 2 (2023-2028) Inspection Date	Next Planned SCP Insp. Date	PC1 Inspection Duration (hrs)	PC2 Inspection Duration (hrs)	Urgent Maint- enance	Inspection Notes / Maintenance-Repair Recommendation
695	Dry Detention Pond	Dry Detention Pond	59960040	City of Platteville, Chestnut Street, Rountree Branch Bridge B-22-270	Energy Dissipator Basin	Grant	Southwest	CTH	D	Local							
696	Dry Detention Pond	Dry Detention Pond	13900474	Jefferson Bypass, STH 89 - Jahn Road	Detention Basin 2	Jefferson	Southwest	STH	26	State		5/6/2024	5/1/2029		1.00	0	Monitor standing water in pipe at IP1, dense cattails and vegetation at IP9, and sediment accumulation and
717	Wet Detention Pond	Wet Detention Pond	10301172	N-S Freeway - CTH K Int., Mainline and Ramps	Storm Water Detention Pond NW	Racine	Southeast	IH	94	State	6/15/2023		5/1/2028	0.25		0	Monitor Endwall #1 which is ~50% clogged with sediment and vegetation.
718	Infiltration Basin	Infiltration Basin	64140080	Village of Whiting, Post Road,, Tommy's Turnpike to Elm Street	Infiltration Basin 1	Portage	North Central	Bus	51	State	8/15/2022		5/1/2027			0	
719	Infiltration Basin	Infiltration Basin	64140080	Village of Whiting, Post Road,, Tommy's Turnpike to Elm Street	Infiltration Basin 2	Portage	North Central	Bus	51	State	8/15/2022		5/1/2027			0	
720	Stormwater Quality Vault	Concrete Vault	64140080	Village of Whiting, Post Road,, Tommy's Turnpike to Elm Street	Grit Chamber Type C	Portage	North Central	Bus	51	State			5/1/2025	0.00			Would require coordination with local maintenance due to being in the median of a high traffic roadway.
721	Wet Detention Pond	Wet Detention Pond	11970070	Spooner - Minong, Trego Interchange, USH 53/63	Pond D	Washburn	Northwest	USH	53	State		6/24/2025	5/1/2025	0.00			
722	Wet Detention Pond	Wet Detention Pond	11970070	Spooner - Minong, Trego Interchange, USH 53/63	Pond B	Washburn	Northwest	USH	53	State		6/24/2025	5/1/2025	0.00			
723	Wet Detention Pond	Wet Detention Pond	11970070	Spooner - Minong, Trego Interchange, USH 53/63	Pond USH 63	Washburn	Northwest	USH	63	State		6/24/2025	5/1/2025	0.00			
724	Retired																
725	Retired																
726	Wet Detention Pond	Pond RB	27830570	CTH M to STH 190	Pond RB	Waukesha	Southeast	STH	190	Local			5/1/2025	0.00			Hans Confirmed Local 06/04/25
727	Wet Detention Pond	Pond LL	27830570	CTH M to STH 190	Pond LL	Waukesha	Southeast	STH	190	Local			5/1/2025	0.00			Hans Confirmed Local 06/04/25
728	Wet Detention Pond	Pond FV	27830570	CTH M to STH 190	Pond FV	Waukesha	Southeast	STH	190	Local			5/1/2025	0.00			Hans Confirmed Local 06/04/25
729	Wet Detention Pond	Drainage Pond	27880071	South Leg of West Waukesha ByPass	Wet Detention Pond	Waukesha	Southeast	USH	18	State	8/14/2023		5/1/2028	0.50		0	
730	Bioretention Basin	Bioretention	27880071	South Leg of West Waukesha ByPass	Bioretention Pond	Waukesha	Southeast	USH	18	State	8/14/2023		5/1/2028	0.50		0	
731	Retired																
732	Swale	Grassed Swale	36380073	C Lake Mills, Mulberry Street; E Madison Street To E Grant Street	Swale	Jefferson	Southeast	LOC STR	Mulberry Street	Local				0.00			
733	Wet Detention Pond	Wet Detention Pond	37600070	Cth H, Cth Kr To Braun Rd; Cth Kr To Braun Rd	Pond G	Racine	Southeast	CTH	H	Local	6/6/2025		5/1/2025	0.00			Hans Confirmed Local 06/04/25
734	Infiltration Basin	Infiltration Basin	14300077	City Of Montello, Underwood Avenue; Park Street To West Street	Infiltration Basin	Marquette	Northcentral	STH	23	State	7/8/2024		5/1/2029		0.50	0	Remove cattails blocking downstream drainage. Monitor: standing water at IP1, IP2 and main basin
735	Swale	Swale	59900038	C Janesville, Ruger Avenue; Wright Road To Ush 14	Drainage Swale	Rock	Southwest	CTH	MM	Local			5/1/2025	0.00			Hans confirmed local 7/21/2025
736	Wet Detention Pond	Wet Detention Pond	12290471	City Glendale, N Port Washington Rd; Bender Rd To W Daphne Rd	Brown Deer Pond	Milwaukee	Southeast	IH	43	State		8/6/2024	5/1/2029		0.23	1	2024 Urgent Maintenance: Eroded embankment about IP 1 and broken endwall on left side when looking
737	Dry Pond	Dry Detention Pond	10606270	East West Freeway; Grandview Boulevard Salt Shed	Detention Pond	Waukesha	Southeast	IH	94	Local				0.00			
738	Wet Detention Pond	Wet Detention Pond	27040076	Wisconn Valley Way, V Mt Pleasant; Cth KR To STH 11	Detention Pond K	Racine	Southeast	LOC STR	Wisconn Valley Way	Local			5/1/2025	0.00			Hans Confirmed Local 06/04/25
739	Wet Detention Pond	Wet Detention Pond	27040076	Wisconn Valley Way, V Mt Pleasant; Cth KR To STH 11	Detention Pond J	Racine	Southeast	LOC STR	Wisconn Valley Way	Local			5/1/2025	0.00			Hans Confirmed Local 06/04/25
740	Wet Detention Pond	Wet Detention Pond	27040076	Wisconn Valley Way, V Mt Pleasant; Cth KR To STH 11	Detention Pond I	Racine	Southeast	LOC STR	Wisconn Valley Way	State			5/1/2025	0.00			Hans Confirmed State Owned 06/04/25
741	Dry Pond	Dry Detention Pond	27040076	Wisconn Valley Way, V Mt Pleasant; Cth KR To STH 11	Pond NE	Racine	Southeast	LOC STR	East Frontage Road	State	8/2/2024		5/1/2029		0.53	0	Remove sediment at IP2, monitor vegetation at IP1
742	Retired													0.00			
743	Retired													0.00			
744	Wet Detention Pond	Wet Detention Pond	27040971	Braun Road; Cth H To 90th Street	Pond Q	Racine	Southeast	LOC STR	Braun Road	Local			5/1/2025	0.00			Hans Confirmed Local 06/04/25
745	Dry Pond	Dry Detention Pond	37600071	CTH H, Braun Rd to STH 11	Pond P	Racine	Southeast	STH	11	Local			5/1/2025	0.00			Hans Confirmed Local 06/04/25
746	Wet Detention Pond	Wet Detention Pond	10603394	Zoo Ic, Swan & Discovery Rndabt; 1060-33-73 Limits To Wdnr Access Rd	Storm Water Pond	Milwaukee	Southeast	LOC STR	Swan Blvd	Local			8/2/2024	0.00			Hans confirmed 8/6/2024 this device is for a local road; not WisDOT responsibility.
747	Dry Pond	Dry Detention Pond	46572503	T Grand Chute, CTH CA; CTH CB - Casaloma Drive	Dry Detention Pond	Outagamie	Northeast	CTH	CA	Local			5/1/2025	0.00			
748	Dry Pond	Dry Detention Pond	58490202	C Fitchburg, Mckee Road; Commerce Park Dr To Seminole Hwy	SEMINOLE VILLAGE POND	Dane	Southwest	CTH	PD	Local			5/1/2025	0.00			
749	Dry Pond	Dry Detention Pond	58490202	C Fitchburg, Mckee Road; Commerce Park Dr To Seminole Hwy	Commerce Park Drive Detention	Dane	Southwest	CTH	PD	Local				0.00			Hans noted 05/30/2024 this is a City of Fitchburg Pond, we don't have to inspect it.
750	Retired																
751	Retired																
752	Retired																
753	Retired																
754	Wet Detention Pond	Wet Detention Pond	92661103	V Ashwaubanon, Vanderperren Way; Holmgren Way - Ashland Avenue	Detention Pond	Brown	Northeast	CTH	HH	Local			5/1/2025	0.00			
755	Wet Detention Pond	Wet Detention Pond	10603478	Zoo Ic, Detention Pond; At North Avenue Interchange	North Avenue Detention Pond/ Ramp NOB Pond	Milwaukee	Southeast	NON HWY	North Avenue	State	6/4/2024		5/1/2029		1.25	1	2024 Urgent Maintenance: Large patches of embankment seepage need stabilization. Repair rilling at edge of riprap. Monitor illicit discharge, stabilize erosion mat
756	Wet Detention Pond	Wet Detention Pond	27590370	North Avenue; Calhoun Road To E County Line	Wet Detention Pond	Waukesha	Southeast	CTH	M	Local			5/1/2025	0.00			Hans Confirmed Local 06/04/25
757	Biofiltration device	Biofilter	27590370	North Avenue; Calhoun Road To E County Line	Biofilter Pond	Waukesha	Southeast	CTH	M	Local			5/1/2025	0.00			Hans Confirmed Local 06/04/25
758	Wet Detention Pond	Wet Detention Pond	37630074	CTH KR, V Mt Pleasant; CTH H To Old Greenbay Road	Pond L	Racine	Southeast	CTH	KR	State	6/6/2025		5/1/2025	0.00			Hans Confirmed State 06/04/25
759	Wet Detention Pond	Wet Detention Pond	37630074	CTH KR, V Mt Pleasant; CTH H To Old Greenbay Road	Pond M	Racine	Southeast	CTH	KR	State	6/6/2025		5/1/2025	0.00			Hans Confirmed State 06/04/25
760	Wet Detention Pond	Wet Detention Pond	37630074	CTH KR, V Mt Pleasant; CTH H To Old Greenbay Road	Pond N	Racine	Southeast	CTH	KR	State	6/6/2025		5/1/2025	0.00			Hans Confirmed State 06/04/25
761	Wet Detention Pond	Wet Detention Pond	37630074	CTH KR, V Mt Pleasant; CTH H To Old Greenbay Road	Regenerative Stormwater	Kenosha	Southeast	CTH	KR	State	8/2/2024		5/1/2029		0.37	0	Monitor: undercutting at IP4, vegetation at IP5
762	Wet Detention Pond	Wet Detention Pond	92001071	Shawano - Green Bay; CTH VV Interchange	CTH VV Detention Pond	Brown	Northeast	STH	29	Local			5/1/2025	0.00			Hans Confirmed Local 06/04/25
763	Wet Detention Pond	Wet Detention Pond	92001071	Shawano - Green Bay; CTH VV Interchange	CTH TS Detention Pond	Brown	Northeast	STH	29	Local			5/1/2025	0.00			Hans Confirmed Local 06/04/25
764	Wet Detention Pond	Wet Detention Pond	92001071	Shawano - Green Bay; CTH VV Interchange	Evergreen Avenue Detention	Brown	Northeast	STH	29	Local			5/1/2025	0.00			Hans Confirmed Local 06/04/25
765	Swale	Infiltration Basin	72000273	C River Falls, Sth 35 And Sth 65; Division St/Cth M (Hoffman Place)	Infiltration Basin	St Croix	Northwest	STH	35	Local				0.00			
766	Dry Pond	Infiltration Basin	72000273	C River Falls, Sth 35 And Sth 65; Division St/Cth M (Hoffman Place)	Infiltration Basin	St Croix	Northwest	STH	35	Local				0.00			
767	Dry Pond	Drainage Swale	72000273	C River Falls, Sth 35 And Sth 65; Division St/Cth M (Hoffman Place)	Drainage Swale	St Croix	Northwest	STH	35	State	6/23/2025		5/1/2025	0.00			
768	Wet Detention Pond	Wet Detention Pond	13101070	75th St, C Kenosha/V Pleasant Prair; Ih 94 To 74th Ave	Pond Chateau	Kenosha	Southeast	STH	50	State	8/2/2024		5/1/2029		0.47	0	No Comments
769	Wet Detention Pond	Pond	13101070	75th St, C Kenosha/V Pleasant Prair; Ih 94 To 74th Ave	Pond GB	Kenosha	Southeast	STH	50	State	8/2/2024		5/1/2029		0.65	0	Monitor erosion at IP6
770	Wet Detention Pond	Wet Detention Pond	13101070	75th St, C Kenosha/V Pleasant Prair; Ih 94 To 74th Ave	Pond 3/ Pond DZ	Kenosha	Southeast	STH	50	State		6/6/2025	5/1/2025	0.00			Hans Confirmed State Owned 06/04/25
771	Swale	Ditch	38760571	V Oakfield, Cth D; Cth Y - N. School Street	N School's Ditch	Fond Du Lac	Northeast	CTH	D	Local			5/1/2025	0.00			Hans confirmed local 7/21/2025
772	Swale	Drainage Ditch	38760571	V Oakfield, Cth D; Cth Y - N. School Street	North Drainage Ditch	Fond Du Lac	Northeast	CTH	D	Local			5/1/2025	0.00			Hans confirmed local 7/21/2025

SCP_ID	SCP_TY	Final SCP Name	PROJ_ID	PROJ_DESC	SCP_DESC	CNTY	DOT_RGN	ROAD_SYS	ROAD_N M	Maintenance Responsibility	Permit Cycle 1 (2018-2023) Inspection Date	Permit Cycle 2 (2023-2028) Inspection Date	Next Planned SCP Insp. Date	PC1 Inspection Duration (hrs)	PC2 Inspection Duration (hrs)	Urgent Maint- enance	Inspection Notes / Maintenance-Repair Recommendation
773	Swale	Bioswale	21500071	Brown Deer Rd C Milw & V Brwn Deer; 91st Street To Deerbrook Trail	Bioswale Area #1	Milwaukee	Southeast	STH	100	State		6/20/2025	5/1/2025	0.00			
774	Swale	Bioswale	21500071	Brown Deer Rd C Milw & V Brwn Deer; 91st Street To Deerbrook Trail	Bioswale Area #2	Milwaukee	Southeast	STH	100	State		6/20/2025	5/1/2025	0.00			
775	Dry Pond	Infiltration Basin	64140079	Village Of Plover, Post Road; Springville Dr To Tommy'S Turnpike	Infiltration Basin #1	Portage	Northcentral	BUS	51	State		8/13/2024	5/1/2029		0.08	0	No Comments
776	Dry Pond	Infiltration Basin	64140079	Village Of Plover, Post Road; Springville Dr To Tommy'S Turnpike	Infiltration Basin #2	Portage	Northcentral	BUS	51	State		8/13/2024	5/1/2029		0.13	0	Monitor sediment buildup at IP1, IP2, and IP3
777	Dry Pond	Dry Detention	11467571	Sth 76-New London; Wi Central Rr-Cth Jj	Dry Pond 387 LT	Outagamie	Northeast	STH	15	State		7/24/2024	5/1/2029		0.42	0	No Comments
778	Dry Pond	Dry Detention	11467571	Sth 76-New London; Wi Central Rr-Cth Jj	Dry Pond 387 RT	Outagamie	Northeast	STH	15	State		7/24/2024	5/1/2029		0.25	0	No Comments
779	Dry Pond	Dry Detention	11467571	Sth 76-New London; Wi Central Rr-Cth Jj	Dry Pond 394	Outagamie	Northeast	STH	15	State		7/24/2024	5/1/2029		0.30	0	Monitor scour pool DS of IP1, trees in riprap near outlet device.
780	Dry Pond	Dry Detention	11467571	Sth 76-New London; Wi Central Rr-Cth Jj	Dry Pond 402	Outagamie	Northeast	STH	15	State		7/24/2024	5/1/2029		0.50	0	Monitor bare soil patch in main basin area.
781	Dry Pond	Dry Detention Pond	0	Poynette Salt Storage Facility; N3243 Co Rd J	Dry Stormwater Basin	Columbia	Southwest	Co Rd	J	State		10/15/2024	5/1/2029		0.50	0	No Comments
782	Vegetated Filter Strip	Filter Strip	0	Edgerton Salt Storage Facility; 1015 CTH A	Filter Strips	Dane	Southwest	CTH	A	State		9/9/2024	5/1/2029		0.50	0	Work Needed: Restore vegetation where temporary sediment trap is located.
783	Dry Pond	Dry Detention Pond	0	Edgerton Salt Storage Facility; 1015 CTH A	Dry Detention Pond	Dane	Southwest	CTH	A	State		9/9/2024	5/1/2029		0.50	0	Monitor: No vegetation or riprap at IP1
784	Bioretention Basin	Bioretention Pond	10100289	Madison - Portage; Dane County Salt Storage Facility	Bioretention Basin	Dane	Southwest	USH	51	State		8/15/2024	5/1/2029		0.50	0	Work Needed: Add Riprap to SE Outlet, add barrier around pea gravel pile. Monitor: Vegetation overgrowth, bare soil and rilling along fence line located north of fence line
785	Dry Pond	Dry Detention Pond	0	Columbus Salt Storage Facility; 1274 Greenway Rd	Dry Stormwater Basin	Dane	Southwest	Co Rd	V	State		8/15/2024	5/1/2029		0.50	0	Work Needed: Remove animal carcass, fill eroded gully, remove sediment, regrade grassy area south of the storage garage. Monitor: Vegetation overgrowth, gulying, rusty endwall, cattail growth.
786	Vegetated Filter Strip	Filter Strip	11070074	Lomira Salt Storage Facility; 300 East Ave	Filter Strip	Dodge	Southwest	USH	41	State		10/15/2024	5/1/2029		0.50	0	Monitor: Vegetation along edge of pavement is dead
787	Wet Detention Pond	Wet Detention Pond	63500689	Junction City Salt Storage Facility; 2660 Co Rd HH	Bioretention Basin	Portage	Northcentral	USH	10	State		10/21/2024	5/1/2029		0.50	0	Monitor: Invasive vegetation and bare soil
788	Bioretention device	Biofilter	0	Parking Lot Reconstruction NW Region HQ; Superior WI; 1701 N. 4th St	Parking Lot Biofilters	Douglas	Northwest	LOC STR	N. 4th St	State		9/3/2024	5/1/2029		0.50	0	Work Needed: Remove vegetation and sediment accumulation in bio-filter curb cuts
789	Wet Detention Pond	Wet Detention Pond	11900080	Chippewa Falls - Eau Claire; USH 53 Interchange; Melby St	Detention Pond (East)	Chippewa	Northwest	USH	53	State		6/18/2025	5/1/2025	0.00			
790	Wet Detention Pond	Wet Detention Pond	11900080	Chippewa Falls - Eau Claire; USH 53 Interchange; Melby St	Detention Pond (West)	Chippewa	Northwest	USH	53	State		8/14/2024	5/1/2029		0.92	0	Remove trees in and around IP1. Remove tree growth at IP2. Monitor joint separation at IP1.
791	Wet Detention Pond	Wet Detention	12282271	Ih 43 North South Freeway; Capitol Dr 2100 Ft N Of Hampton Ave	Lake Tower Pond	Milwaukee	Southeast	IH	43	State		8/14/2024	5/1/2029		0.58	0	Repair: erosion at embankment. Monitor bare spots
792	Dry Detention Pond	Dry Drainage	36700171	C Columbus, Farnham Street - 0.32 mi S of Avalon RD to Park Ave	Hoffman Pond	Columbia	Southwest	STH	89	State			1/1/2026	0.00			
793	Wet Detention Pond	Wet Detention	50340072	Blanchardville - Dane County Line (STH 78 to STH 39)	Sedimentation Basin	Iowa	Southwest	CTH	F	State			1/1/2026	0.00			
794	Wet Detention Pond	Wet Detention	12282271	Ih 43 North South Freeway; Capitol Dr 2100 Ft N Of Hampton Ave	Oak Leaf Pond	Milwaukee	Southeast	IH	43	State		8/14/2024	5/1/2029		0.37	0	No Comments
795	Wet Detention Pond	Wet Detention	37320971	CTH K, 60th St, UPRR - 94th Ct	Stormwater Detention Pond	Kenosha	Southeast	CTH	K	State			1/1/2026	0.00			
796	Wet Detention Pond	Wet Detention	65600871	Kaukauna - Seymour, CTH JJ Intersection	Private Pond	Outagamie	Northeast	CTH	JJ	Private				0.00			
797	Infiltration Basin	Sedimentation Basin	51630777	Genoa - La Crosse, Sunnyside Dr - Garner Pl	Sediment Basin	La Crosse	southwest	STH	35	State			1/1/2026	0.00			
798	Infiltration Basin	Infiltration Basin	56820075	Cth Q - Cth I; Otter Creek Bridge, B-25- 0191	Infiltration Basin	Iowa	Southeast	CTH	II	Private				0.00			
799	Infiltration Basin	Energy Dissipator	15600070	Spooner - Hayward; Greenwood Ave To Poplar Street	Energy Dissipator Basin +1258	Washburn	Northwest	USH	63	State			1/1/2026	0.00			
800	Infiltration Basin	Energy Dissipator	15600070	Spooner - Hayward; Greenwood Ave To Poplar Street	Energy Dissipator Basin +1310	Washburn	Northwest	USH	63	State			1/1/2026	0.00			
801	Wet Detention Pond	Wet Detention	12290474	I-43 North South Freeway; W County Line Rd Interchange	Port Washington Rd Pond	Milwaukee	Southeast	IH	43	State		8/6/2024	5/1/2029		0.43	0	Erosion above endwall at IP 1, IP 1 pipe damage, Debris blocking Outlet Device 1
802	Bioswale	Bioswale Infiltration	27220970	C New Berlin S Calhoun Road; Intersection With Small Road	Bioswale	Waukesha	Southeast	LOC STR	S Calhoun Rd S Lincoln	Local				0.00			
803	Infiltration Trench	Trench	69951270	C Marshfield, S Lincoln Avenue; West 5th Street To Adler Road	Infiltration Trench	Wood	North Central	LOC STR	Ave	Local				0.00			
804	Wet Detention Pond	Wet Detention	12290475	I-43 North South Freeway; Mequon Rd Interchange	Mequon Road South Pond	Ozaukee	Southeast	IH	43	State			1/1/2028	0.00	0	Construction in progress as of 8/6/2024	
805	Wet Detention Pond	Wet Detention	12290475	I-43 North South Freeway; Mequon Rd Interchange	Mequon Road North Pond	Ozaukee	Southeast	IH	43	State			1/1/2028	0.00	0	Construction in progress as of 8/6/2024	
806	Wet Detention Pond	Wet Detention	20401470	Lovers Lane (City of Franklin); Rawson Ave To College Ave	Storm Water Detention Pond	Milwaukee	Southeast	USH	45	State			1/1/2028	0.00			
807	Wet Detention Pond	Wet Detention	1371280	Oconomowoc to Pewaukee Road	Existing Pond	Waukesha	Southeast	STH	16	State			1/1/2027	0.00			
808	Wet Detention Pond	Wet Detention	30800140	Madison - Cambridge; Cth Ab Overpass	Pond A	Dane	Southwest	USH	12	Local			1/1/2027	0.00			
809	Wet Detention Pond	Wet Detention	30800140	Madison - Cambridge; Cth Ab Overpass	Pond B	Dane	Southwest	USH	12	Local			1/1/2027	0.00			
810	Wet Detention Pond	Wet Detention	30800140	Madison - Cambridge; Cth Ab Overpass	Pond C	Dane	Southwest	USH	12	State			1/1/2027	0.00			
811	Wet Detention Pond	Wet Detention	30800140	Madison - Cambridge; Cth Ab Overpass	Pond D	Dane	Southwest	USH	12	State			1/1/2027	0.00			
812	Dry Detention Pond	Dry Detention	13100470	75th Street, Village of Paddock Lake, 256th Avenue - 236th Avenue	Dry Detention Pond	Kenosha	Southeast	STH	50	State			1/1/2027	0.00			
813	Infiltration Basin	STH 47 Storm	92310772	Woodruff - Lac Du Flambeau, Stormwater Drainage Improvements	STH 47 Storm Basin	Vilas	North Central	STH	47	State			1/1/2027	0.00			
814	Bioretention Basin	Bioretention	75750773	C. LaCrosse, LaCrosse Street, Oakland Street to Losey Boulevard	Bioretention basin 200A	La Crosse	southwest	STH	16	Connecting Hwy				0.00			
815	Bioretention Basin	Bioretention	75750773	C. LaCrosse, LaCrosse Street, Oakland Street to Losey Boulevard	Bioretention basin 300A	La Crosse	southwest	STH	16	Connecting Hwy				0.00			
816	Bioretention Basin	Bioretention	75750773	C. LaCrosse, LaCrosse Street, Oakland Street to Losey Boulevard	Bioretention basin 300B	La Crosse	southwest	STH	16	Connecting Hwy				0.00			
817	Bioretention Basin	Bioretention	75750773	C. LaCrosse, LaCrosse Street, Oakland Street to Losey Boulevard	Bioretention basin (unnamed 1)	La Crosse	southwest	STH	16	Connecting Hwy				0.00			
818	Bioretention Basin	Bioretention	75750773	C. LaCrosse, LaCrosse Street, Oakland Street to Losey Boulevard	Bioretention basin (unnamed 2)	La Crosse	southwest	STH	16	Connecting Hwy				0.00			
819	Bioretention Basin	Bioretention	75750773	C. LaCrosse, LaCrosse Street, Oakland Street to Losey Boulevard	Bioretention basin 900A	La Crosse	southwest	STH	16	Connecting Hwy				0.00			
820	Bioretention Basin	Bioretention	75750773	C. LaCrosse, LaCrosse Street, Oakland Street to Losey Boulevard	Bioretention basin 900B	La Crosse	southwest	STH	16	Connecting Hwy				0.00			
821	Bioretention Basin	Bioretention	75750773	C. LaCrosse, LaCrosse Street, Oakland Street to Losey Boulevard	Bioretention basin 900C	La Crosse	southwest	STH	16	Connecting Hwy				0.00			
822	Bioretention Basin	Bioretention	75750773	C. LaCrosse, LaCrosse Street, Oakland Street to Losey Boulevard	Bioretention basin 900D	La Crosse	southwest	STH	16	Connecting Hwy				0.00			
823	Bioretention Basin	Bioretention	75750773	C. LaCrosse, LaCrosse Street, Oakland Street to Losey Boulevard	Bioretention basin 900E	La Crosse	southwest	STH	16	Connecting Hwy				0.00			
824	Bioretention Basin	Bioretention	75750773	C. LaCrosse, LaCrosse Street, Oakland Street to Losey Boulevard	Bioretention basin 900F	La Crosse	southwest	STH	16	Connecting Hwy				0.00			
825	Wet Detention Pond	Wet Detention	53100275	Spring Green - Madison; Pleasant View Road Intersection	Pond D	Dane	Southwest	USH	14	Local				0.00			
826	Wet Detention Pond	Wet Detention	53100275	Spring Green - Madison; Pleasant View Road Intersection	Pond E	Dane	Southwest	USH	14	Local				0.00			
827	Wet Detention Pond	Wet Detention	53100275	Spring Green - Madison; Pleasant View Road Intersection	Pond F	Dane	Southwest	USH	14	Local				0.00			
828	Dry Detention Pond	Stormwater Detention Pond	10200180	Hudson - Baldwin, IH 94 SWEF 61 Hudson	Stormwater Detention Pond	St. Croix	Northeast	IH	94	State		8/8/2024	5/1/2029		0.58	0	Remove rock bags out IP1. Monitor: standing water at IP1, ponding near Outlet Device, vegetation growth near Outlet Device.
829	Swale	Retention Swale Stormwater	63550074	CTH H - CTH F, Edgewood Avenue to CTH F	Outfall 1	Adams	Southwest	CTH	Z	Local				0.00			
830	Swale	Retention Swale Stormwater	63550074	CTH H - CTH F, Edgewood Avenue to CTH F	Outfall 2	Adams	Southwest	CTH	Z	Local				0.00			
831	Swale	Retention Swale	63550074	CTH H - CTH F, Edgewood Avenue to CTH F	Outfall 3	Adams	Southwest	CTH	Z	Local				0.00			
832	Wet Detention Pond	Wet Detention	40300170	Summit Drive - Spring Street	Storm Water Detention Pond	Ozaukee	Southeast	STH	33	Local				1.00			
833	Wet Detention Pond	Wet Detention	11306476	Appleton - De Pere, Northland/STH 15 Intchg B440315/16	Northland Ramp B Pond (NLPB)	Outagamie	Northeast	IH	41	State			1/1/2028	0.00			

SCP_ID	SCP_TY	Final SCP Name	PROJ_ID	PROJ_DESC	SCP_DESC	CNTY	DOT_RGN	ROAD_SYS	ROAD_N M	Maintenance Responsibility	Permit Cycle 1 (2018-2023) Inspection Date	Permit Cycle 2 (2023-2028) Inspection Date	Next Planned SCP Insp. Date	PC1 Inspection Duration (hrs)	PC2 Inspection Duration (hrs)	Urgent Maint- enance	Inspection Notes / Maintenance-Repair Recommendation
834	Wet Detention Pond	Wet Detention	11306476	Appleton - De Pere, Northland/STH 15 Intchg B440315/16	Northland Ramp D Pond (NLPD)	Outagamie	Northeast	IH	41	State			1/1/2028	0.00			
835	Infiltration Basin	Infiltration Basin	67670171	V Plover, CTH B, Wisconsin Avenue to Hoover Avenue	Dry Detention Area	Portage	Northcentral	CTH	B	Local							
836	Enhanced Grass Swale	Enhanced Grass	10101082	Madison – Portage, Wisconsin River Bridges	Permanent Sediment Basin	Columbia	Southwest	IH	39	State			1/1/2028	0.00			
837	Biofilter	Bioinfiltration Basin	79940051	C River Falls, S Wasson Lane, 830th Ave to E Cascade Ave	Bioinfiltration Basin 1	Pierce	Northwest	LOC STR		S Wasson Ln Local							
838	Biofilter	Bioinfiltration Basin	79940051	C River Falls, S Wasson Lane, 830th Ave to E Cascade Ave	Bioinfiltration Basin 2	Pierce	Northwest	LOC STR		S Wasson Ln Local							
839	Dry Detention Pond	Dry Detention	10710780	Lacrosse – Sparta, Safety Rest Area 16 Sparta - Site	Rest Area Stormwater Drainage	Monroe	Southwest	IH	90	State			1/1/2029	0.00			
840	Wet Detention Pond	Wet Detention Pond	11306881	Appleton - De Pere, Sbc Early Fill and Frontage Roads	Southbridge Rd Ramp B Pond	Brown	Northeast	LOC STR		Southbrid ge Rd State			1/1/2029	0.00			
841	Wet Detention Pond	Wet Detention Pond	11306881	Appleton - De Pere, Sbc Early Fill and Frontage Roads	Southbridge Rd Ramp D Pond	Brown	Northeast	LOC STR		Southbrid ge Rd State			1/1/2029	0.00			
842	Wet Detention Pond	Wet Detention Pond	58490207	C Fitchburg, Syene Road, Mccoy Road Intersection	McCoy Road Pond	Dane	Southwest	LOC STR		Syene Rd Local							
843	Wet Detention Pond	Wet Detention Pond	11304474	Appleton- Green Bay, IH 41 Wrightstown Swef 34/Post-Bldg	Stormwater Detention Basin	Outagamie	Northeast	IH	41	State			1/1/2029	0.00			
844	Wet Detention Pond	Wet Detention Pond	11306472	Appleton - De Pere, I-41 Mainline, Rr Bridge-Lyndale	Lynndale Pond Quadrant C (Pond APC)	Outagamie	Northeast	IH	41	State			1/1/2029	0.00			
845	Wet Detention Pond	Wet Detention Pond	42320071	C Sheboygan, Weeden Creek Road, CTH Ok to S 12th Street	Pond	Sheboygan	Northeast	CTH	EE	Local							
846	Wet Detention Pond	Wet Detention Pond	27480171	Pewaukee Road; CTH J - Rockwood Drive to STH 190 & STH 164 - STH 190 to Swan Road	CTH M Pond	Waukesha	Southeast	STH	164	State	6/20/2025	5/1/2025	0.00				
847	Wet Detention Pond	Wet Detention Pond	27590370	North Avenue; Calhoun Road To E County Line	Wet Detention Pond	Waukesha	Southeast	CTH	M	Local			5/1/2025	0.00			Hans Confirmed Local 06/04/25
848	Enhanced Grass Swale	Bioswale	27590370	North Avenue; Calhoun Road To E County Line	Bioswale	Waukesha	Southeast	CTH	M	State	6/20/2025	5/1/2025	0.00				
849	Enhanced Grass Swale	Bioswale	27590370	North Avenue; Calhoun Road To E County Line	Bioswale	Waukesha	Southeast	CTH	M	State	6/20/2025	5/1/2025	0.00				
850	Enhanced Grass Swale	Bioswale	27590370	North Avenue; Calhoun Road To E County Line	Bioswale	Waukesha	Southeast	CTH	M	State	6/20/2025	5/1/2025	0.00				

Section I – Typical Highway and Freeway Cross Sections

PROJECT

STATEWIDE
STORMWATER
MAPPING
MANAGEMENT

0601-00-40
MASTER CONTRACT ID M0697-01-74
WORK ORDER number 011

CLIENT

WISCONSIN
DEPARTMENT OF
TRANSPORTATION

BUREAU OF TECHNICAL SERVICES
HILL FARMS STATE TRANSPORTATION
BUILDING
4802 SHEBOYGAN AVE.
MADISON, WISCONSIN 53707
608-267-3147 tel
wisconsin.gov

CONSULTANT

AECOM
1350 DEMING WAY SUITE 100
MIDDLETON, WI 53562
608-836-9800 tel
608-836-9767 fax
www.aecom.com
SUB-CONSULTANTS

REGISTRATION

ISSUE/REVISION

I/R	DATE	DESCRIPTION

KEY PLAN

PROJECT NUMBER

60530309

SHEET TITLE

TYPICAL CROSS SECTIONS FOR
HIGHWAY WATER QUALITY
ANALYSIS

SHEET NUMBER

ANSI D 22" x 34"

Approved:

Checked:

Designer: K/W

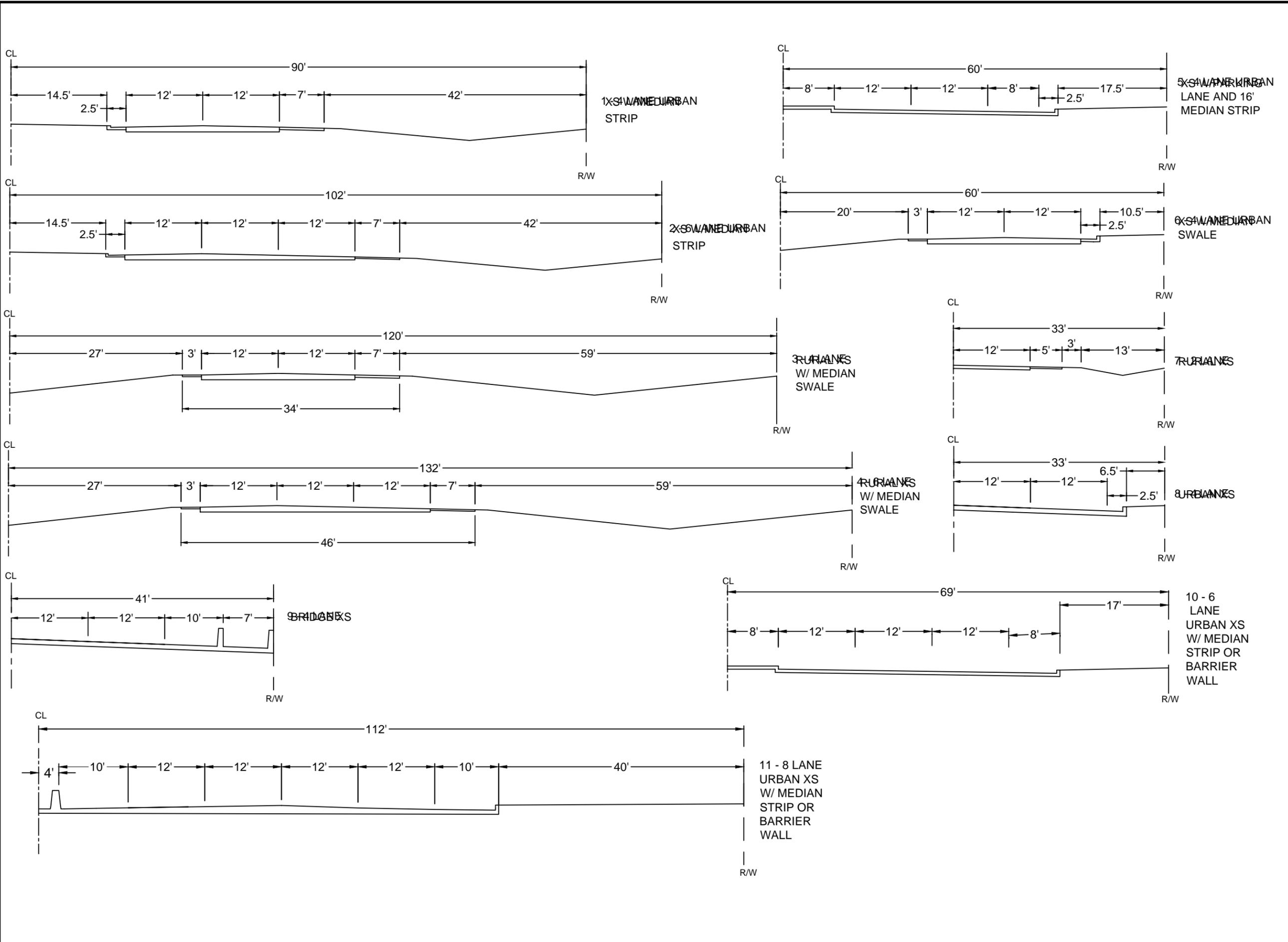
Project Management Initials:

1.DWG TD 2.DWG

10:48

11X17

11X17



Plotted By: tabatha.davis
Plot File Date Created: -ug/28/2019 10:48 -
Layout--Sheet Name: 11X17
Filename: C:\USERS\T-B-TH-D- ISV-PPD-T-LOC-L\TEMP-CPUBLISH_65992\TYPIC-L CROSS SECTIONS 1.DWG TD 2.DWG

Standard Highway and Freeway Cross Sections

Cross Section Number	Cross Section Description	SLU Code	Total Cross Section Width (ft)	Paved Lane and Shoulder Width (ft)	Grassed Area Width (ft)	Paved Lane and Shoulder Area for 100 total acres (ac)	Grassed Area for 100 total acres (ac)	Area Adjustment to Account for Interchanges/ Intersections	Adj. Paved Lane and Shoulder Area for 100 total acres	Adj. Grassed Area for 100 total acres (ac)	Highway Length for 100 acres (mi)	Highway Length for 100 acres (ft)	Swale Length Multiplier	Swale Length for 100 acres (ft)	Total Swale Length/ac (Swale Density)	ADT Range
1	4 Lane Urban XS with Median Strip	HTU Type 1	180	67	113	37.22	62.78	1.86	39.08	60.92	4.58	24200	2	48400	484	
2	6 Lane Urban XS w/Median Strip	HTU Type 2	204	91	113	44.61	55.39	2.23	46.84	53.16	4.04	21353	2	42706	427	50000
3	4 Lane Rural XS with Median Swale	HTU Type 3	240	68	172	28.33	71.67	0.57	28.90	71.10	3.44	18150	3	54450	544.5	7500/ 15000/ 30000
4	6 Lane Rural XS w/Median Swale	HTU Type 4	264	92	172	34.85	65.15	0.70	35.55	64.45	3.13	16500	3	49500	495	
5	4 Lane Urban XS w/Parking Lane and 16' Median Strip	HTU Type 5	120	85	35	70.83	29.17	3.54	74.38	25.63	6.88	36300	0	0	0	15000
6	4 Lane Urban XS with Median Swale	HTU Type 6	120	59	61	49.17	50.83	2.46	51.63	48.38	6.88	36300	1	36300	363	
7	2 Lane Rural XS	HTU Type 7	66	34	32	51.52	48.48	1.03	52.55	47.45	12.50	66000	2	132000	1320	
8	4 Lane Urban XS	HTU Type 8	66	53	13	80.30	19.70	4.02	84.32	15.68	12.50	66000	0	0	0	3500
9	4 Lane Bridge	HTU Type 9	82	82	0	100.00	0.00	0.00	100.00	0.00	10.06	53121.95	0	0	0	
10	6 Lane Urban XS w/Median Strip or Barrier Wall	HTU Type 10	118	112	6	94.92	5.08	4.75	99.66	0.34	6.99	36915.25	0	0	0	
11	8 Lane Urban XS w/Median Strip or Barrier Wall	HTU Type 11	224	144	80	64.29	35.71	3.21	67.50	32.50	3.68	19446.43	0	0	0	
								0.02	Rural Area Adjustment Factor							
								0.05	Urban Area Adjustment Factor							

Section J – List of Non-Highway WisDOT SWPPP Facilities

Facility Name	Address	Facility Manager	Facility Manager Email	Local Site Contact	Lat/long
DTS (Division of Transportation System Development)					
SW Region HQ, Madison, Truax Complex	2101 Wright St. Madison, WI 53704	Alyssa Lynch	alyssa.lynych@dot.wi.gov	alyssa.lynych@dot.wi.gov	43.12728029216422, -89.32886158486139
Wisconsin Rapids Sign Shop	2841 Industrial St. Wisconsin Rapids, WI 54495	Mark Ratty	mark.ratty@dot.wi.gov	ryan.schindhelm@dot.wi.gov douglas.trudell@dot.wi.gov	44.40139754336488, -89.85895701041855
West Allis Service Facility	935 S. 60th St. West Allis, WI 53214	Joshua Bargar	joshua.bargar@dot.wi.gov	joshua.bargar@dot.wi.gov	43.021953789817125, -87.9881547812322
West Allis Garage					
West Allis Cold Storage					
La Crosse Sign Shop	3550 Mormon Coulee Rd. LaCrosse, WI 54601	Kevin Wellnitz	kevin.wellnitz@dot.wi.gov	eric.glinning@dot.wi.gov samuel.kube@dot.wi.gov	43.779036848241695, -91.22549952014485
La Crosse Electric Storage					
La Crosse Cold Storage Bldg					
Eau Claire Sign Shop	5009 Hwy 53 South Eau Claire, WI 54701	Peter Young	peter.young@dot.wi.gov	stacey.rusch@dot.wi.gov	44.76182763144157, -91.41330621888746
Eau Claire Vehicle Maint Bldg					
Eau Claire Cold Storage Bldg					
NC Regional Office, Rhinelander	501 Hanson Lake Rd. Rhinelander, WI 54501	Mark Ratty	mark.ratty@dot.wi.gov	ryan.schindhelm@dot.wi.gov nickp.krueger@dot.wi.gov steven.hunter@dot.wi.gov	45.6347681469012, -89.44482056966494
Rhinelander Sign Shop					
Rhinelander Garage					
Rhinelander Storage Shed					
NE Regional HQ, Green Bay	944 Vanderperren Way Green Bay, WI 54304	Emily Simmons	emily.simmons@dot.wi.gov	chad.degrave@dot.wi.gov	44.47898298395113, -88.07615475333968
Green Bay Garage					
Green Bay Cold Storage Bldg					
Green Bay Shop Maint Bldg					
NW Regional Office, Superior	1701 N 4th St. Superior, WI 54880	Peter Young	peter.young@dot.wi.gov	brooke.gustafson@dot.wi.gov	46.734304895201916, -92.10243169218553
Superior Cold Storage Bldg	W7074 Green Valley Rd. Spooner, WI 54801	Mike Goeser	michael.goeser@dot.wi.gov	mark.woody@dot.wi.gov	45.807835573279625, -91.89719256630761
Spooner Vehicle Maint Bldg					
Salt Storage Facilities					
Poynette Dome	N3243 County Road J, Poynette, WI 53955	Ryan Murray	ryan.murray@dot.wi.gov peter.kaiser@dot.wi.gov		
Verona Shed	4391 Maple Grove Drive, Madison, WI 53719	Tammy Buss	tammy.buss@dot.wi.gov		
Edgerton Shed	1015 County Hwy A, Edgerton, WI 53534				
DeForest Shed	6155 US-51, DeForest, WI 53532				
Columbus Hi-Arch	1274 Greenway Rd, Columbus, WI 53925	Ryan Murray	ryan.murray@dot.wi.gov stefan.ciobanu@dot.wi.gov		
Watertown Dome	N1233 County Road L, Watertown, WI 53098				
Lomira Hi-Arch	300 East Ave, Lomira, WI 53048	Randy Franks	randy.franks@dot.wi.gov		
Brodhead Dome	N2397 County Rd Gg, Brodhead, WI 53520	Terry Lammert	terry.lammert@dot.wi.gov		
Arena Dome	7440 US-14, Arena, WI 53503	Ken Leinweber	kenneth.leinweber@dot.wi.gov		
Lake Mills Hi-Arch	W7645 Oasis Ln, Lake Mills, WI 53551	Terry Lammert	terry.lammert@dot.wi.gov		
La Crosse Shed	3028 County Rd SS, Onalaska, WI 54650				
Janesville Shed	1903 E LaPrairie Lane, Janesville, WI 53546				
Milton Hi-Arch	6153 N Co Rd M, Milton, WI 53563	Terry Lammert	terry.lammert@dot.wi.gov		
Milton Shed					
Evansville Shed	9216 US-14, Evansville, WI 53536				
Kenosha Dome	5195 120th Ave, Kenosha, WI 53144	Jason Jilling	jason.jilling@dot.wi.gov		
Chippewa Falls Shed	1371 122nd St. Lake Hallie, WI 54729	Erik Hendrickson	erik.hendrickson@dot.wi.gov		
Millston Shed	WisDOT Rest Area 53, W7955 I-94, Black River Falls, WI 54615	Nick Carroll	nichol.carroll@dot.wi.gov		
Houlton Shed	1349 Main St, Houlton, WI 54082	Mike Burke	michael.burke@dot.wi.gov		
Ashwaubenon Hi-Arch	3401 Spirit Way, Ashwaubenon, WI 54304				
Newton Shed	Wisconsin State Patrol Sub Station, Newton, WI 53063	Matthew Haefs	matthew.haefs@dot.wi.gov		
Oshkosh Hi-Arch	1409 WI-26, Oshkosh, WI 54904				
Junction City Shed	2260 County Rd HH, Junction City, WI 54443	Bryan Fay	bryan.fay@dot.wi.gov		

Note: SWPPPs will be sent to WDNR as a link to the stormwater management web page.

Section K – Milwaukee River Bacteria TMDL Pilot Study Report

Milwaukee River Basin Bacteria TMDL Study Drainage Memo

Map and Inventory of Possible Sources

Prepared for:



WisDOT

4822 Madison Yards Way
P.O. Box 7965
Madison, WI 53707-7965

Prepared by:

AECOM

Lu Chen
Emma Connell, EIT

Reviewed by:
John Voorhees

AECOM
1555 N River Center Dr,
Milwaukee, WI 53212

aecom.com

January 2025
WisDOT Project Number 0656-50-08
AECOM Project Number 60735734

Table of Contents

Study Overview.....	2
1. GIS Map Development.....	2
2. Desktop Analysis.....	6
3. Field Investigation	7
4. Cost Analysis	7
5. Lessons Learned	7
6. Results of the Pilot Study Field Investigation	8
7. Recommended Next Steps	8
Appendix 1: WPDES Permit No. WI-S066800-2, Section 3.5.....	9
Appendix 2: Desktop Analysis Spreadsheets.....	10
Appendix 3: Completed Investigation Forms	11

Study Overview

The Wisconsin Department of Transportation (WisDOT) is required by WDNR WPDES General Permit No. WI-S066800-2, Transportation Separate Storm Sewer System (TS4) Permit Section 3.5.1 (see **Appendix 1**), to create a map and inventory of possible bacteria (fecal coliform and E. coli) sources in the Milwaukee River Total Maximum Daily Load (TMDL) watershed that drain to or are adjacent to WisDOT owned highways, excluding connecting highways. Further, the TS4 requires WisDOT to develop a bacteria elimination plan if sources are found.

WisDOT is using a step-by-step approach to meet these requirements. WisDOT's first step was to direct AECOM, using MC 0697-03-34, WO 02, to develop a process and tools to identify possible bacteria sources in the Milwaukee River watershed as a pilot to study a portion of the watershed. The pilot study area included 80 miles of the 265 miles (approximately one-quarter) of WisDOT owned highways within the watershed area. The pilot included tracking the time and effort to perform the work.

Pilot Study Steps:

1. Develop a GIS map of the Milwaukee River TMDL watershed showing WisDOT roadways and select approximately 80 miles of representative rural, suburban and urban roadways to study.
2. Perform a desktop analysis to locate possible bacteria sources.
3. Develop a spreadsheet inventory of these possible bacteria sources.
4. Perform a field investigation of the selected possible bacteria sources using the investigation form to document findings.
5. List the level of effort and costs to do the work.
6. Summarize the lessons learned from the pilot study.

The pilot study was started in September 2024 and 80 miles were completed by January of 2025. The remaining 185 miles of state-owned highway in the Milwaukee River TMDL watershed is planned to be completed later in 2025 and the results appended to this memo.

1. GIS Map Development

The GIS maps included in this study were created using ArcGIS Pro version 3.2.0. The maps show the Milwaukee River TMDL watershed limits, WisDOT highways, some location elements, portions of highways classified into rural, suburban and urban studied in the pilot and potential bacteria sources needing field investigation.

Eighty miles of highway were selected for the pilot study. The process of selecting the 80 miles of highway was as follows:

1. Select highway segments in rural, suburban and urban areas.
2. Keep as many segments as possible contiguous to make the desktop analysis and site selection process more efficient and reduce travel time for field investigations.
3. Add unique highway sections such as those through small towns and by any Concentrated Animal Feeding Operations (CAFOs)

The 80 miles of highway selected include:

1. 28.6 miles of highway in rural areas (3 segments).
2. 26.9 miles of highway in suburban areas (6 segments).
3. 24.1 miles of highway in urban areas (9 segments).

Figure 1. Milwaukee River TMDL watershed drainage basin showing the Interstates, US Highways and State Trunk Highways.



Figure 2. Milwaukee River TMDL drainage basin showing the 80 miles of highway selected in the pilot study and all the possible bacteria sources identified during the desktop analysis. In this figure, blue represents highways in rural areas, red indicates highways in suburban areas, and purple indicates highways in urban areas. The highway sections are labeled according to the watershed area they're located in, R for rural, SU for suburban, and U for urban. The highway sections are numbered starting from the northern section of the watershed and going south.

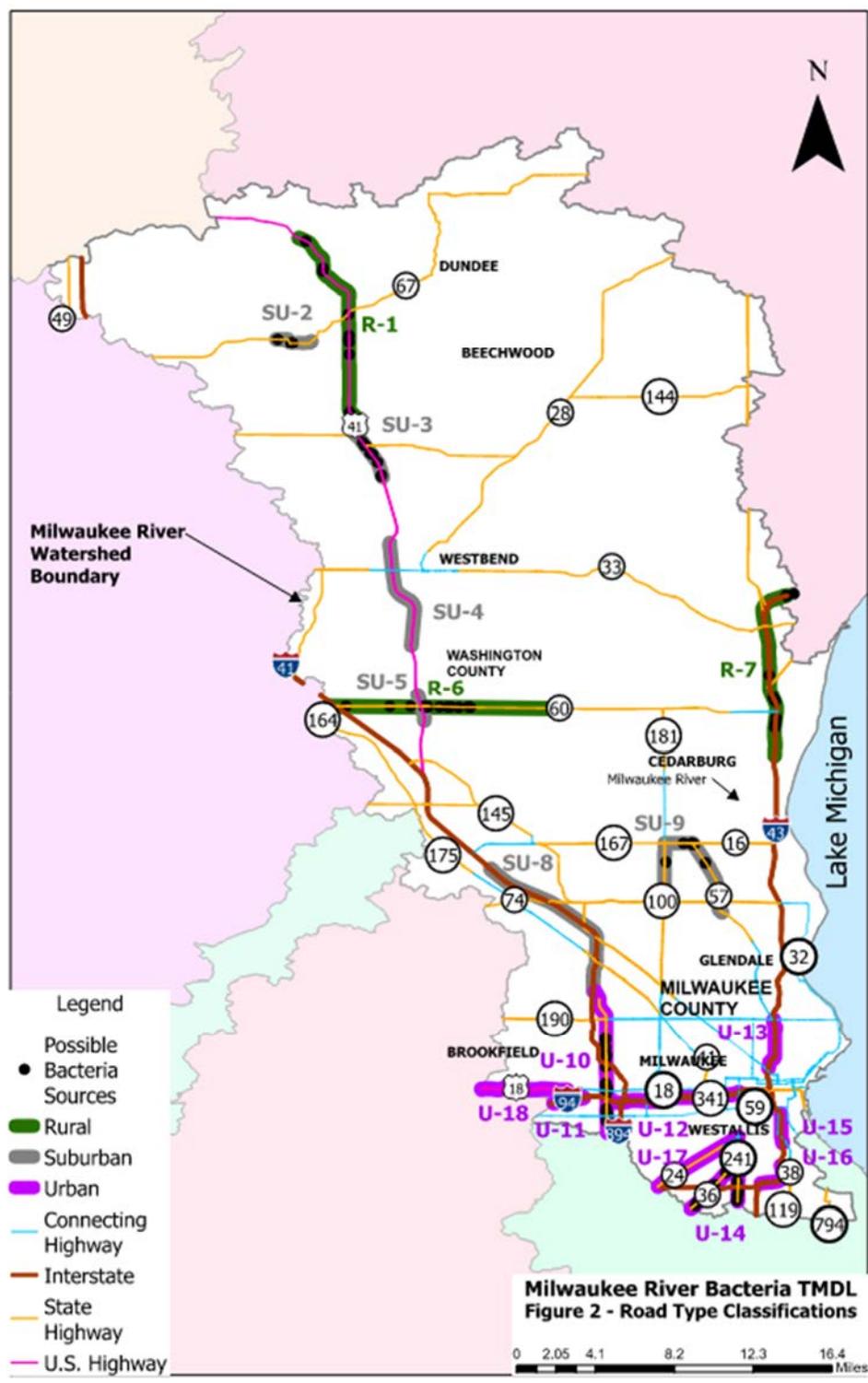
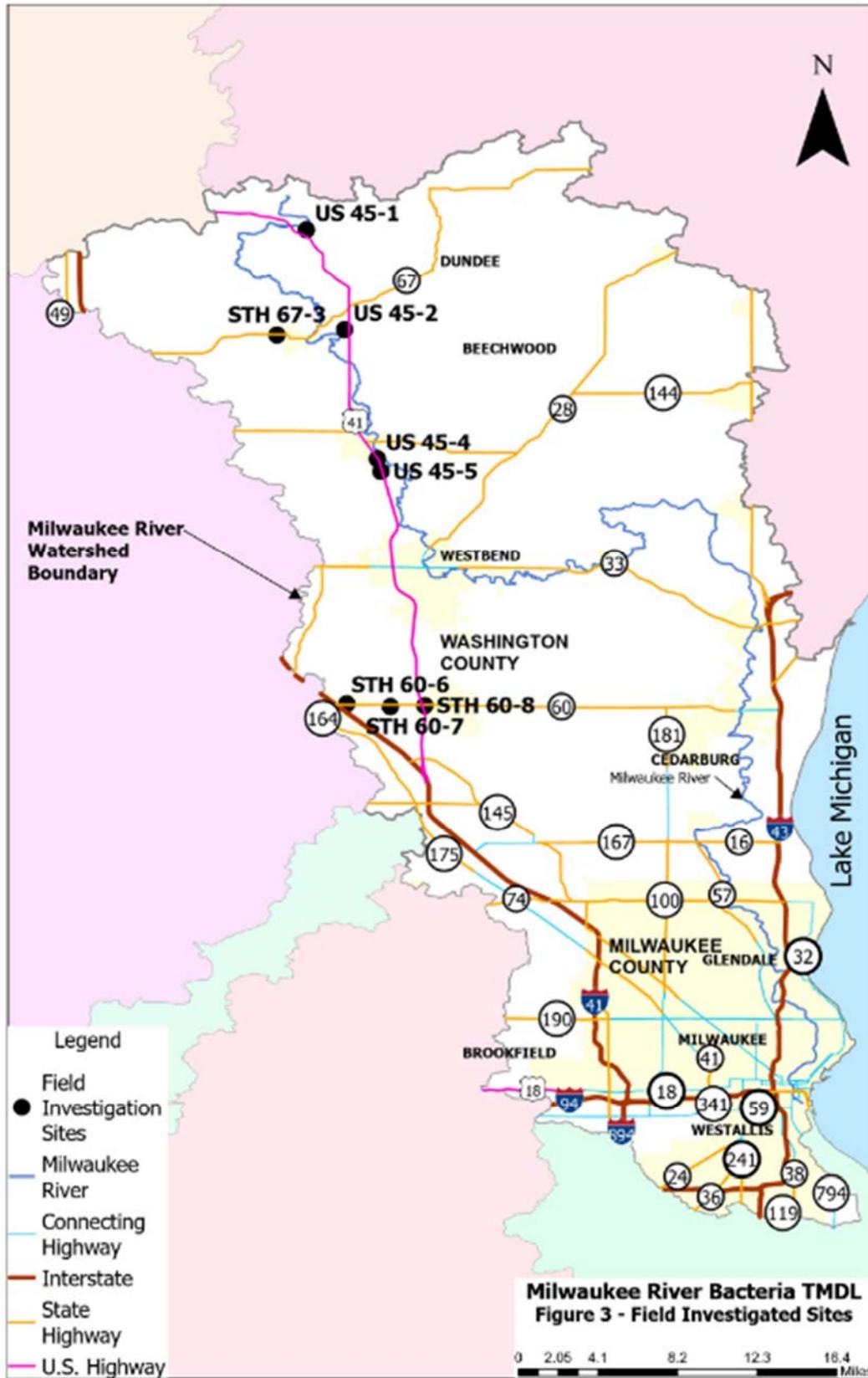


Figure 3. Field investigation sites labeled with the highway name (i.e. U.S. Highway 45 is US-45) along with the corresponding I.D. number. These labels correspond to the labels in **Appendix 2c**.



2. Desktop Analysis

A desktop mapping and viewing analysis was conducted to locate possible bacteria sources along WisDOT owned highways. Using the Google Maps aerial imagery and street view options, AECOM searched for any areas listed in the possible bacteria sources list shown below that were adjacent to the highway. If a possible bacteria source was identified, it was cataloged in the spreadsheet inventory. Further investigation was done using Google Street View and topographic maps to determine if the site drained to the highway right-of-way (ROW). If the site did drain to the highway ROW, it was flagged for field investigation.

Possible Sources of Bacteria

- Sanitary sewer overflow locations
- Livestock housed or raised adjacent to the highway right-of-way
- Manure storage sites
- Zoos
- Kennels
- Animal breeders
- Pet stores
- Dog training facilities
- Waste hauling
- Storage and transfer facilities
- Areas that attract congregations of nuisance urban birds and wildlife
- Known or suspected properties with inadequate food or organic waste handling or storage
- Composting sites or facilities
- CAFOs and barns immediately adjacent to the highway
- Urban parks
- Wet detention ponds
- The Milwaukee County Zoo
- Dead animal transfer locations
- Known or suspected areas with improper human sanitation use
- Fast food restaurants
- Any other sources that the permittee of the DNR has reason to believe are discharging bacteria to the highway right-of-way (none were forwarded to WisDOT by the DNR).

Develop Spreadsheet Inventory

The desktop analysis was recorded in the spreadsheet inventory **Appendix 2. Appendix 2a** is a summary of the desktop Google Street View analysis broken down by the **type of watershed area** (rural, suburban, and urban). There is additional information such as the start location of the analysis, the miles analyzed, and the time it took to analyze this segment of highway in this spreadsheet tab. The cost analysis was developed using the data in this table.

Appendix 2b includes every identified possible bacteria location in the pilot study, along with detailed information such as coordinates, type of source, and screenshots from street view. Following discussion and review with WisDOT, eight locations required field investigations (**Appendix 2c**). The 8 possible bacteria locations were noted in **Appendix 2a** as well.

3. Field Investigation

On October 8, 2024, AECOM staff and the WisDOT BTS-ESS Statewide Stormwater Quality Engineer, completed a field investigation of the eight possible bacteria sources identified from the desktop analysis that drained to the highway right-of-way. Each site review was conducted visually and only from the public ROW, avoiding any entry onto private properties. Observational assessments focused on the surrounding area drainage to the highway ROW and where possible bacteria sources may be on the site (i.e. dog play areas, grease traps, etc.). Photos of the site and ROW area were taken and an investigation form completed for each site.

The investigation form described the possible bacteria site, where the bacteria source is located and if it drains to the ROW. It includes space for site photos.

During this investigation, no indicators of bacterial contamination were identified across the eight sites. Each investigation took approximately 22 minutes per site, not including driving time. The completed investigation forms are in **Appendix 3**.

4. Cost Analysis

The pilot study costs include the raw hourly rates for two inspectors plus overhead and fixed fees. The direct costs include total mileage plus lunches for two inspectors. The total cost includes an additional 5% to account for project management. The final cost estimate is \$573 per mile of highway (\$45,820 total cost/80 miles studied). If the balance of the TMDL were evaluated the cost estimate for the remaining 185 miles would be \$105,959 (\$573 per mile * 185 miles).

No.	Task	Time Spent (hours)	Cost
1	GIS setup	20	\$1,900
2	Desktop Analysis	67	\$5,490
3	Field Investigation (80 miles)	16	\$1,310
4	Document preparation & Report/Memo	100	\$8,190
5	Meeting & Discussion	9	\$780
6	Direct Costs	Travel Mileage/Lunch 2x	\$170
7	Overhead Costs		\$22,160
8	Fixed Fee		\$3,640
	Totals (including project management)		\$45,820

5. Lessons Learned

ArcGIS Setup

The possible bacteria sites identified for field investigation are numbered first by highway name and then by numbered going north to south and east to west. For example, the most northeast site on US Highway 45 is US 45-1 and the most southwest site on State Highway 60 is STH 60-8.

Desktop Analysis

Perform a keyword search on Google Maps using terms such as "pet," "vet," "grooming", etc. at the beginning of each section analysis. This approach can save time and provide an initial indication of whether the selected section contains any apparent possible bacteria sources. It is also crucial to conduct a detailed review using Google Street View, as there may be unnamed farms or animal-related areas with possible bacterial sources that might be overlooked. Utilize Google Earth or GIS tools to obtain detailed elevation data if determining the water flow direction from Google Maps proves challenging. These tools provide precise topographic information that can help accurately assess flow paths. Typically, a half mile or less was found to be a manageable length of roadway to perform the detailed desktop analysis.

Field Investigation

This task should be done with two staff to ensure efficiency and staff safety. One person should take photos and the other should fill out the investigation form. Take notes to describe the flow path going from the possible bacteria site to the ROW.

6. Results of the Pilot Study Field Investigation

The eight field sites did not reveal any signs of bacteria draining into the WisDOT owned ROW. The sites either had sufficient lawns that acted as a filter strip for any bacteria before getting to the ROW or the possible sources of bacteria (e.g. animal walking areas) were well maintained and there was no evidence of bacteria from the site draining to the ROW.

7. Recommended Next Steps

1. Develop a new Work Order Contract to perform the bacteria study for the rest of the roadways in the Milwaukee River Watershed.
2. Share the results with the DNR as part of the TS4 Report.
3. If potential bacteria sources are found when the entire watershed is studied, develop a bacteria management plan. This may involve coordination with local units of government.

Appendix 1: WPDES Permit No. WI-S066800-2, Section 3.5

TMDL Compliance and Implementation for Bacteria Waste Load Allocations (WLA).

3.5 TMDL Compliance and Implementation for Bacteria WLAs. This section applies to TMDL reachsheds with a bacteria WLA specified in the Milwaukee River Basin TMDL Final Report dated March 19, 2018. DOT shall comply with the following: By September 30, 2026, the DOT shall develop and submit to the Department an inventory of bacteria sources and a map indicating the locations of the possible sources of fecal coliform and *E. coli* entering its TS4. The inventory shall be in a tabular format and include a label code, the name of the source, the physical address or location description of the source, and the ownership of the source (i.e. public or private). The map shall be to scale, identify all streets within the TS4, and indicate the locations of the sources using the label codes. The DOT shall consider the variation in flow conditions in its identification of possible sources. The inventory and map shall include the following possible sources of bacteria such as:

- Sanitary sewer overflow locations.
- Livestock housed or raised adjacent to the TS4 permitted area and discharging to the TS4, but not including household pets.
- Zoos, kennels, animal breeders, pet stores, and dog training facilities.
- Waste hauling, storage, and transfer facilities.
- Areas that attract congregations of nuisance urban birds and wildlife.
- Known or suspected properties with inadequate food or organic waste handling or storage.
- Composting sites or facilities.
- Known or suspected areas with improper human sanitation use.
- Any other source that the permittee or the Department has a reason to believe is discharging bacteria to the TS4.

3.5.2. By September 30, 2027, the DOT shall collaborate with adjacent municipalities to submit to the Department a proposed bacteria source elimination plan. The plan shall consist of a strategy and prioritization scheme to eliminate each source of bacteria identified under section 3.5.1. The plan shall include:

- A copy of the evidence provided to the adjacent MS4.
- How DOT is collaborating with the MS4 containing the source to eliminate it.
- If appropriate, add filter strips or other green infrastructure practice to break the flow path and intercept the bacteria sources.
- What education was provided to the owner of the source area and how it was provided.

Note: While the TMDL allocations in the Milwaukee River Basin TMDL are expressed only in terms of fecal coliform, both fecal coliform and *E. coli* have been listed as sources of recreational use impairments that the TMDL was completed to address.

Appendix 2: Desktop Analysis Spreadsheets

Appendix 2a - Desktop Analysis Logs for Rural Drainage Areas

Highway Segment No. Analyzed	Segment Number	Highway Name	Type of Highway	Start Location	GIS Source	Subarea	Miles	Desktop Analysis Time (Minutes)	Notes
1	R-1	US Highway 45	US Highway	US 45 & 3938-3816 County Rd W	Google Maps	Rural	0.32	26	US 45-1
2	R-1	US Highway 45	US Highway	43.673095, -88.270547	Google Maps	Rural	0.33	21	-
3	R-1	US Highway 45	US Highway	43.669565, -88.268019	Google Maps	Rural	0.43	30	-
4	R-1	US Highway 45	US Highway	Mitchell Rd 43.666649, -88.266030	Google Maps	Rural	0.30	17	-
5	R-1	US Highway 45	US Highway	Schultz Family Farms N3132 US-45, Eden, WI 53019	Google Maps	Rural	0.45	22	-
6	R-1	US Highway 45	US Highway	43.650352, -88.259693	Google Maps	Rural	0.33	21	-
7	R-1	US Highway 45	US Highway	Dave's Auto & Truck N2935 US-45, Campbellsport, WI	Google Maps	Rural	0.49	32	-
8	R-1	US Highway 45	US Highway	Woodland Dr 43.644852, -88.253117	Google Maps	Rural	0.36	26	-
9	R-1	US Highway 45	US Highway	Happy Hollow Rd 43.637673, -88.243214	Google Maps	Rural	0.51	24	-
10	R-1	US Highway 45	US Highway	43.636121, -88.241360	Google Maps	Rural	0.36	22	-
11	R-1	US Highway 45	US Highway	Valley Hydro Excavation LLC N 2496, N2496 US-45,	Google Maps	Rural	0.35	19	-
12	R-1	US Highway 45	US Highway	43.623319, -88.240980	Google Maps	Rural	0.32	35	-
13	R-1	US Highway 45	US Highway	Lovers Rd & Jim Pfantz Concrete Contractor	Google Maps	Rural	0.38	26	-
14	R-1	US Highway 45	US Highway	OHenry's Service N1776 US-45, Campbellsport, WI	Google Maps	Rural	0.35	29	US 45-2
15	R-1	US Highway 45	US Highway	Sunset Dr 43.601289, -88.240646	Google Maps	Rural	0.41	26	-
16	R-1	US Highway 45	US Highway	43.593914, -88.240612	Google Maps	Rural	0.36	22	-
17	R-1	US Highway 45	US Highway	43.589232, -88.240598	Google Maps	Rural	0.39	28	-
18	R-1	US Highway 45	US Highway	Gatzke Ln, 43.579337, -88.240582	Google Maps	Rural	0.38	24	-
19	R-1	US Highway 45	US Highway	Haven Dr & Christy Ln 43.572110, -88.240546	Google Maps	Rural	0.42	21	-
20	R-1	US Highway 45	US Highway	Auburn Ashford Dr 43.564818, -88.240525	Google Maps	Rural	0.37	25	-
21	R-1	US Highway 45	US Highway	Old BridGE Rd, 43.557505, -88.240597	Google Maps	Rural	0.34	22	-
22	R-1	US Highway 45	US Highway	Sandy Rd 43.550218, -88.240709	Google Maps	Rural	0.42	28	-
23	R-1	US Highway 45	US Highway	43.544671, -88.238424	Google Maps	Rural	0.38	21	-
24	R-1	US Highway 45	US Highway	Co Hwy V 43.532524, -88.236068	Google Maps	Rural	0.37	29	-
25	R-6	State Highway 60	State Highway	43.324522, -88.257033	Google Maps	Rural	0.49	31	-
26	R-6	State Highway 60	State Highway	Tillie Lake Veterinary Clinic 2015 American Eagle Dr, Slinger,	Google Maps	Rural	0.34	24	STH 60-6
27	R-6	State Highway 60	State Highway	43.324479, -88.225335	Google Maps	Rural	0.34	22	-
28	R-6	State Highway 60	State Highway	Scenic Rd 43.324507, -88.221458	Google Maps	Rural	0.38	25	-
29	R-6	State Highway 60	State Highway	Pet Resort 3473 WI-60 Trunk, Jackson, WI	Google Maps	Rural	0.33	23	STH 60-7
30	R-6	State Highway 60	State Highway	Lily Rd 43.324552, -88.206940	Google Maps	Rural	0.35	24	-
31	R-6	State Highway 60	State Highway	S Mayfield Rd 43.324535, -88.202120	Google Maps	Rural	0.30	20	-
32	R-6	State Highway 60	State Highway	John's A-1 Auto Body 43.324487, -88.194546	Google Maps	Rural	0.33	21	-
33	R-6	State Highway 60	State Highway	Tillie Rd 43.324471, -88.190800	Google Maps	Rural	0.36	27	-
34	R-6	State Highway 60	State Highway	Pharmacal Way 43.324507, -88.184384	Google Maps	Rural	0.34	21	-
35	R-6	State Highway 60	State Highway	Pharmacal Way 43.324342, -88.184384	Google Maps	Rural	0.32	10	-
36	R-6	State Highway 60	State Highway	Pharmacal Way 43.324299, -88.183708	Google Maps	Rural	0.35	21	STH 60-8
37	R-6	State Highway 60	State Highway	3747-3731 Co Hwy P 43.324516, -88.183236	Google Maps	Rural	0.43	29	-
38	R-6	State Highway 60	State Highway	43.324249, -88.173066	Google Maps	Rural	0.42	34	-

Appendix 2a - Desktop Analysis Logs for Suburban Drainage Areas

Highway Segment No. Analyzed	Segment Number	Highway Name	Type of Highway	Start Location	GIS Source	Subarea	Miles	Desktop Analysis Time (Minutes)	Notes
1	SU-3	US Highway 45	US Highway	43.532071, -88.236230	Google Maps	Suburban	0.38	22	-
2	SU-3	US Highway 45	US Highway	43.528458, -88.235106	Google Maps	Suburban	0.45	27	-
3	SU-3	US Highway 45	US Highway	43.521078, -88.228967	Google Maps	Suburban	0.42	23	-
4	SU-3	US Highway 45	US Highway	43.513882, -88.223724	Google Maps	Suburban	0.56	38	-
5	SU-3	US Highway 45	US Highway	43.506529, -88.218330	Google Maps	Suburban	0.63	35	US 45-5
6	SU-3	US Highway 45	US Highway	43.448584, -88.209398	Google Maps	Suburban	0.57	10	US 45-4
7	SU-4	US Highway 45	US Highway	43.443507, -88.209834	Google Maps	Suburban	0.39	12	-
8	SU-4	US Highway 45	US Highway	43.432580, -88.208738	Google Maps	Suburban	0.45	25	-
9	SU-4	US Highway 45	US Highway	43.427383, -88.208277	Google Maps	Suburban	0.44	23	-
10	SU-4	US Highway 45	US Highway	43.421085, -88.207334	Google Maps	Suburban	0.56	29	-
11	SU-4	US Highway 45	US Highway	43.412806, -88.205722	Google Maps	Suburban	0.55	28	-
12	SU-4	US Highway 45	US Highway	43.408029, -88.201224	Google Maps	Suburban	0.55	21	-
13	SU-4	US Highway 45	US Highway	43.403371, -88.194032	Google Maps	Suburban	0.37	19	-
14	SU-4	US Highway 45	US Highway	43.397760, -88.191214	Google Maps	Suburban	0.43	21	-
15	SU-4	US Highway 45	US Highway	43.393995, -88.191522	Google Maps	Suburban	0.38	19	-
16	SU-5	US Highway 45	US Highway	43.332399, -88.189313	Google Maps	Suburban	0.43	20	-
17	SU-5	US Highway 45	US Highway	43.324369, -88.186437	Google Maps	Suburban	0.46	25	-
18	SU-5	US Highway 45	US Highway	43.309579, -88.184830	Google Maps	Suburban	0.49	23	-
19	SU-2	State Highway 67	State Highway	N Baumann St 43.601140, -88.295406	Google Maps	Suburban	0.36	21	STH 67-3
20	SU-2	State Highway 67	State Highway	N Railroad St 43.601086, -88.289284	Google Maps	Suburban	0.45	26	-
21	SU-2	State Highway 67	State Highway	N Fond Du Lac Ave 43.601076, -88.286972	Google Maps	Suburban	0.42	28	-
22	SU-2	State Highway 67	State Highway	S Fond Du Lac Ave 43.598131, -88.285200	Google Maps	Suburban	0.42	25	-
23	SU-2	State Highway 67	State Highway	S Helena St 43.598017, -88.276561	Google Maps	Suburban	0.41	21	-
24	SU-2	State Highway 67	State Highway	S Jenny St 43.597997, -88.274442	Google Maps	Suburban	0.38	20	-
25	SU-8	State Highway 41	State Highway	43.204381, -88.136649	Google Maps	Suburban	0.85	38	-
26	SU-8	State Highway 41	State Highway	43.192540, -88.122417	Google Maps	Suburban	0.49	25	-
27	SU-8	State Highway 41	State Highway	43.187803, -88.113338	Google Maps	Suburban	0.65	36	-
28	SU-8	State Highway 41	State Highway	43.181536, -88.098987	Google Maps	Suburban	0.44	26	-
29	SU-8	State Highway 41	State Highway	43.176358, -88.087492	Google Maps	Suburban	0.46	18	-
30	SU-8	State Highway 41	State Highway	43.173100, -88.082131	Google Maps	Suburban	0.72	39	-
31	SU-8	State Highway 41	State Highway	43.160267, -88.064065	Google Maps	Suburban	0.49	28	-
32	SU-8	State Highway 41	State Highway	43.146899, -88.054698	Google Maps	Suburban	0.41	27	-
33	SU-8	State Highway 41	State Highway	43.126479, -88.056599	Google Maps	Suburban	0.55	30	-
34	SU-9	Interstate 181	Interstate	43.170786, -88.004446	Google Maps	Suburban	0.43	31	-
35	SU-9	Interstate 181	Interstate	43.177771, -88.005319	Google Maps	Suburban	0.40	24	-
36	SU-9	Interstate 181	Interstate	43.180816, -88.004218	Google Maps	Suburban	0.42	25	-
37	SU-9	Interstate 181	Interstate	43.192558, -88.003678	Google Maps	Suburban	0.52	21	-

Highway Segment No. Analyzed	Segment Number	Highway Name	Type of Highway	Start Location	GIS Source	Subarea	Miles	Desktop Analysis Time (Minutes)	Notes
38	SU-9	Interstate 181	Interstate	43.201945, -88.003905	Google Maps	Suburban	0.67	35	-
39	SU-9	Interstate 181	Interstate	43.207120, -88.003593	Google Maps	Suburban	0.42	27	-
40	SU-9	Interstate 181	Interstate	43.218300, -88.003550	Google Maps	Suburban	0.63	31	-
41	SU-9	Interstate 167	Interstate	43.221621, -88.003530	Google Maps	Suburban	0.51	30	-
42	SU-9	Interstate 167	Interstate	43.221559, -87.993914	Google Maps	Suburban	0.45	23	-
43	SU-9	Interstate 167	Interstate	43.221565, -87.989645	Google Maps	Suburban	0.48	25	-
44	SU-9	Interstate 57	Interstate	43.221509, -87.982662	Google Maps	Suburban	0.69	37	-
45	SU-9	Interstate 57	Interstate	43.217223, -87.979983	Google Maps	Suburban	0.56	31	-
46	SU-9	Interstate 57	Interstate	43.214460, -87.978081	Google Maps	Suburban	0.57	31	-
47	SU-9	Interstate 57	Interstate	43.209467, -87.974604	Google Maps	Suburban	0.39	24	-
48	SU-9	Interstate 57	Interstate	43.204764, -87.971294	Google Maps	Suburban	0.40	20	-
49	SU-9	Interstate 57	Interstate	43.198495, -87.967482	Google Maps	Suburban	0.42	23	-
50	SU-9	Interstate 57	Interstate	43.192488, -87.964561	Google Maps	Suburban	0.51	35	-
51	SU-9	Interstate 57	Interstate	43.182794, -87.963358	Google Maps	Suburban	0.43	22	-
52	SU-9	Interstate 57	Interstate	43.178395, -87.961662	Google Maps	Suburban	0.34	23	-
53	SU-9	Interstate 57	Interstate	43.171991, -87.963156	Google Maps	Suburban	0.38	24	-
Totals							25.64	1370	

Appendix 2a - Desktop Analysis Logs for Urban Drainage Areas

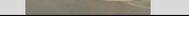
Highway Segment No. Analyzed	Segment Number	Highway Name	Type of Highway	Start Location	GIS Source	Subarea	Miles	Desktop Analysis Time (Minutes)	Notes
23	U-10	State Highway 100	State Highway	43.110798, -88.055578	Google Maps	Urban	0.45	22	-
24	U-10	State Highway 100	State Highway	43.104749, -88.052782	Google Maps	Urban	0.39	18	-
25	U-10	State Highway 100	State Highway	43.096936, -88.051072	Google Maps	Urban	0.38	17	-
26	U-10	State Highway 100	State Highway	43.089567, -88.047600	Google Maps	Urban	0.42	21	-
27	U-10	State Highway 100	State Highway	43.085680, -88.047612	Google Maps	Urban	0.34	15	-
28	U-10	State Highway 100	State Highway	43.082129, -88.047634	Google Maps	Urban	0.41	21	-
29	U-10	State Highway 100	State Highway	43.074766, -88.047803	Google Maps	Urban	0.50	27	-
30	U-10	State Highway 100	State Highway	43.067419, -88.047665	Google Maps	Urban	0.51	25	-
31	U-10	State Highway 100	State Highway	43.059885, -88.047545	Google Maps	Urban	0.57	20	-
32	U-10	State Highway 100	State Highway	43.055323, -88.047462	Google Maps	Urban	0.43	19	-
33	U-10	State Highway 100	State Highway	43.045538, -88.047059	Google Maps	Urban	0.70	30	-
34	U-10	State Highway 100	State Highway	43.034925, -88.046968	Google Maps	Urban	0.45	19	-
35	U-10	State Highway 100	State Highway	43.027927, -88.047295	Google Maps	Urban	0.78	32	-
36	U-10	State Highway 100	State Highway	43.016329, -88.046786	Google Maps	Urban	0.58	20	-
37	U-10	State Highway 100	State Highway	43.009890, -88.046571	Google Maps	Urban	0.57	20	-
57	U-11	Interstate 94	Interstate	43.025536, -88.089483	Google Maps	Urban	0.97	28	-
58	U-12	Interstate 94	Interstate	43.027962, -88.031976	Google Maps	Urban	0.84	27	-
59	U-12	Interstate 94	Interstate	43.030453, -87.994076	Google Maps	Urban	0.88	24	-
60	U-12	Interstate 94	Interstate	43.033143, -87.966166	Google Maps	Urban	1.24	32	-
61	U-12	Interstate 94	Interstate	43.034608, -87.947165	Google Maps	Urban	0.92	29	-
15	U-18	US Highway 18	US Highway	43.036589, -88.141480	Google Maps	Urban	0.23	16	-
16	U-18	US Highway 18	US Highway	43.036852, -88.136904	Google Maps	Urban	0.27	20	-
17	U-18	US Highway 18	US Highway	43.036557, -88.131309	Google Maps	Urban	0.22	15	-
18	U-18	US Highway 18	US Highway	43.036594, -88.127099	Google Maps	Urban	0.32	21	-
19	U-18	US Highway 18	US Highway	43.037215, -88.120669	Google Maps	Urban	0.31	20	-
20	U-18	US Highway 18	US Highway	43.036413, -88.114363	Google Maps	Urban	0.38	25	-
21	U-18	US Highway 18	US Highway	43.036139, -88.106654	Google Maps	Urban	0.22	17	-
22	U-18	US Highway 18	US Highway	43.035940, -88.078277	Google Maps	Urban	0.26	21	-
49	U-13	Interstate 43	Interstate	43.089845, -87.921841	Google Maps	Urban	0.26	18	All elevated highway
50	U-13	Interstate 43	Interstate	43.085930, -87.921553	Google Maps	Urban	0.29	19	-
51	U-13	Interstate 43	Interstate	43.057689, -87.924116	Google Maps	Urban	0.21	15	-
52	U-13	Interstate 43	Interstate	43.054852, -87.925152	Google Maps	Urban	0.23	16	-
53	U-13	Interstate 43	Interstate	43.017736, -87.916494	Google Maps	Urban	0.26	18	-
54	U-13	Interstate 43	Interstate	43.000009, -87.916119	Google Maps	Urban	0.21	15	-
55	U-13	Interstate 43	Interstate	42.979424, -87.916406	Google Maps	Urban	0.23	17	-
56	U-13	Interstate 43	Interstate	42.975366, -87.915694	Google Maps	Urban	0.22	16	-
62	U-13	Interstate 94	Interstate	43.089660, -87.921875	Google Maps	Urban	0.69	21	-

Highway Segment No. Analyzed	Segment Number	Highway Name	Type of Highway	Start Location	GIS Source	Subarea	Miles	Desktop Analysis Time (Minutes)	Notes
63	U-13	Interstate 94	Interstate	43.077610, -87.920827	Google Maps	Urban	0.86	23	-
64	U-13	Interstate 94	Interstate	43.057687, -87.924193	Google Maps	Urban	0.84	22	-
38	U-14	State Highway 36	State Highway	42.946992, -87.983551	Google Maps	Urban	0.42	22	-
39	U-14	State Highway 36	State Highway	42.949369, -87.980755	Google Maps	Urban	0.58	23	-
40	U-14	State Highway 36	State Highway	42.951368, -87.978627	Google Maps	Urban	0.76	29	-
41	U-14	State Highway 36	State Highway	42.959956, -87.969420	Google Maps	Urban	0.49	19	-
42	U-14	State Highway 36	State Highway	42.964865, -87.965570	Google Maps	Urban	0.47	17	-
43	U-14	State Highway 36	State Highway	42.973688, -87.957389	Google Maps	Urban	0.54	24	-
44	U-14	State Highway 36	State Highway	42.975499, -87.955636	Google Maps	Urban	0.45	22	-
45	U-14	State Highway 241	State Highway	42.978269, -87.948513	Google Maps	Urban	0.58	22	-
46	U-14	State Highway 241	State Highway	42.974035, -87.948596	Google Maps	Urban	0.48	29	-
47	U-14	State Highway 241	State Highway	42.966791, -87.948717	Google Maps	Urban	0.43	25	-
48	U-14	State Highway 241	State Highway	42.960618, -87.948815	Google Maps	Urban	0.46	25	-
10	U-15	US Highway 38	US Highway	42.977911, -87.909548	Google Maps	Urban	0.28	19	-
11	U-15	US Highway 38	US Highway	42.973767, -87.909599	Google Maps	Urban	0.50	31	-
12	U-15	US Highway 38	US Highway	42.966477, -87.909679	Google Maps	Urban	0.50	30	-
13	U-15	US Highway 38	US Highway	42.959151, -87.909788	Google Maps	Urban	1.15	42	-
14	U-15	US Highway 38	US Highway	42.968002, -87.948749	Google Maps	Urban	0.28	18	-
65	U-15	Interstate 94	Interstate	43.019216, -87.917391	Google Maps	Urban	0.75	19	-
66	U-15	Interstate 94	Interstate	42.996446, -87.915363	Google Maps	Urban	0.93	26	-
67	U-16	Interstate 94	Interstate	42.979433, -87.916373	Google Maps	Urban	0.79	22	-
68	U-16	Interstate 94	Interstate	42.964995, -87.933120	Google Maps	Urban	1.19	34	-
1	U-17	US Highway 24	US Highway	43.000524, -87.948448	Google Maps	Urban	0.45	25	-
2	U-17	US Highway 24	US Highway	42.996286, -87.955531	Google Maps	Urban	0.25	18	-
3	U-17	US Highway 24	US Highway	42.993260, -87.960855	Google Maps	Urban	0.54	34	-
4	U-17	US Highway 24	US Highway	42.988399, -87.969098	Google Maps	Urban	0.41	23	-
5	U-17	US Highway 24	US Highway	42.986041, -87.972701	Google Maps	Urban	0.52	35	-
6	U-17	US Highway 24	US Highway	42.981061, -87.980855	Google Maps	Urban	0.47	24	-
7	U-17	US Highway 24	US Highway	42.973838, -87.992704	Google Maps	Urban	0.35	20	-
8	U-17	US Highway 24	US Highway	42.970289, -87.998149	Google Maps	Urban	0.28	18	-
9	U-17	US Highway 24	US Highway	42.966506, -88.004238	Google Maps	Urban	0.24	16	-
Totals							17.20	821	

Highway Segment No. Analyzed	Segment Number	Highway Name	Type of Highway	Start Location	GIS Source	Subarea	Miles	Desktop Analysis Time (Minutes)	Notes
39	R-6	State Highway 60	State Highway	43.324091, -88.169392	Google Maps	Rural	0.39	30	-
40	R-6	State Highway 60	State Highway	S Center St 43.324006, -88.166592	Google Maps	Rural	0.34	20	-
41	R-6	State Highway 60	State Highway	Jackson Dr 43.323917, -88.162135	Google Maps	Rural	0.38	27	-
42	R-6	State Highway 60	State Highway	Ridgeway Rd 43.323759, -88.155322	Google Maps	Rural	0.35	29	-
43	R-6	State Highway 60	State Highway	Eagle Dr 43.323645, -88.149649	Google Maps	Rural	0.40	30	-
44	R-6	State Highway 60	State Highway	Maple Rd 43.323528, -88.142322	Google Maps	Rural	0.56	35	-
45	R-6	State Highway 60	State Highway	Division Rd 43.323265, -88.122831	Google Maps	Rural	0.44	25	-
46	R-6	State Highway 60	State Highway	Johnson Ln 43.323199, -88.117514	Google Maps	Rural	0.42	27	-
47	R-6	State Highway 60	State Highway	Center Rd 43.323139, -88.112561	Google Maps	Rural	0.38	29	-
48	R-6	State Highway 60	State Highway	Lusan Dr 43.323050, -88.106258	Google Maps	Rural	0.46	29	-
49	R-6	State Highway 60	State Highway	Church Rd 43.322991, -88.102617	Google Maps	Rural	0.60	36	-
50	R-6	State Highway 60	State Highway	Brianwood Dr 43.322734, -88.086632	Google Maps	Rural	0.40	20	-
51	R-6	State Highway 60	State Highway	County Dr 43.322647, -88.082847	Google Maps	Rural	0.44	30	-
52	R-7	Interstate 43	Interstate	43.407938, -87.912368	Google Maps	Rural	0.36	25	-
53	R-7	Interstate 43	Interstate	Co Hwy B 43.411539, -87.890783	Google Maps	Rural	0.45	33	-
54	R-7	Interstate 43	Interstate	43.409387, -87.906476	Google Maps	Rural	0.38	24	-
55	R-7	Interstate 43	Interstate	43.408031, -87.912087	Google Maps	Rural	0.40	22	-
56	R-7	Interstate 43	Interstate	43.405506, -87.920654	Google Maps	Rural	0.47	34	-
57	R-7	Interstate 43	Interstate	43.401633, -87.928022	Google Maps	Rural	0.45	23	-
58	R-7	Interstate 43	Interstate	43.388752, -87.928841	Google Maps	Rural	0.75	35	-
59	R-7	Interstate 43	Interstate	43.375724, -87.926430	Google Maps	Rural	0.63	33	-
60	R-7	Interstate 43	Interstate	E Sauk Rd 43.368321, -87.925578	Google Maps	Rural	0.61	27	-
61	R-7	Interstate 43	Interstate	E Sauk Rd 43.368321, -87.925579	Google Maps	Rural	0.49	22	-
62	R-7	Interstate 43	Interstate	43.353146, -87.925195	Google Maps	Rural	0.47	25	-
63	R-7	Interstate 43	Interstate	43.344555, -87.925037	Google Maps	Rural	0.51	29	-
64	R-7	Interstate 43	Interstate	43.334466, -87.923981	Google Maps	Rural	0.42	18	-
65	R-7	Interstate 43	Interstate	Arrowhead Rd 43.331342, -87.922045	Google Maps	Rural	0.45	29	-
66	R-7	Interstate 43	Interstate	43.328154, -87.920505	Google Maps	Rural	0.48	28	-
67	R-7	Interstate 43	Interstate	43.315675, -87.919912	Google Maps	Rural	0.35	27	-
68	R-7	Interstate 43	Interstate	43.311660, -87.920333	Google Maps	Rural	0.45	30	-
69	R-7	Interstate 43	Interstate	Lakefield Rd 43.296644, -87.920758	Google Maps	Rural	0.43	19	-
Totals							28.31	1778	

Appendix 2b - Possible Bacteria Sites from the Desktop Analysis

ID	Highway Name	Lat	Long	Address/Location	County	City/Village/Town	Rural/Urban/Suburban	Type of Bacteria Source	GIS Source (Google Earth, County/Municipal GIS, WisDOT DOTView)	Date of Field Verification	Google Street View Screen Shots	Google Aerial Map View	Decision
1	US Highway 45	43.52	-88.23	1526 Fond Du Lac Ave, Kewaskum, WI 53040	Washington and Fond du Lac	Village of Kewaskum	Suburban	Restaurant	Google Maps	N/A			Unlikely bacteria source
2	US Highway 45	43.52	-88.23	102 Main St, Kewaskum, WI 53040	Washington and Fond du Lac	Village of Kewaskum	Suburban	Restaurant	Google Maps	N/A			Unlikely bacteria source
3	US Highway 45	43.52	-88.23	1273 Fond Du Lac Ave, Kewaskum, WI 53040	Washington and Fond du Lac	Village of Kewaskum	Suburban	Restaurant	Google Maps	N/A			Unlikely bacteria source
4	US Highway 45	43.52	-88.23	109 Main St, Kewaskum, WI 53040	Washington and Fond du Lac	Village of Kewaskum	Suburban	Restaurant	Google Maps	N/A			Unlikely bacteria source
5	US Highway 45	43.52	-88.22	1041 Fond Du Lac Ave, Kewaskum, WI 53040	Washington and Fond du Lac	Village of Kewaskum	Suburban	Restaurant	Google Maps	N/A			Unlikely bacteria source
6	US Highway 45	43.513990	-88.22	1010 Fond Du Lac Ave, Kewaskum, WI 53040	Washington and Fond du Lac	Village of Kewaskum	Suburban	Restaurant	Google Maps	N/A			Unlikely bacteria source
7	US Highway 45	43.497640	-88.22	102 Badger Rd #45, Kewaskum, WI 53040	Washington	Town of Kewaskum	Rural	Restaurant	Google Maps	N/A			Unlikely bacteria source
8	State Highway 67	43.60	-88.28	116 E Main St, Campbellsport, WI 53010	Fond du Lac	Village of Campbellsport	Suburban	Restaurant	Google Maps	N/A			Unlikely bacteria source
9	State Highway 67	43.60	-88.28	128 E Main St, Campbellsport, WI 53010	Fond du Lac	Village of Campbellsport	Suburban	Restaurant	Google Maps	N/A			Unlikely bacteria source
10	State Highway 67	43.60	-88.28	153 E Main St, Campbellsport, WI 53010	Fond du Lac	Village of Campbellsport	Suburban	Restaurant	Google Maps	N/A			Unlikely bacteria source
11	Interstate 181	43.21	-88.00	7520 W Donges Bay Rd, Mequon, WI 53092	Ozaukee	City of Mequon	Suburban	Restaurant	Google Maps	N/A			Unlikely bacteria source
12	Interstate 167	43.221530	-87.99	6411 W Mequon Rd, Mequon, WI 53092	Ozaukee	City of Mequon	Suburban	Restaurant	Google Maps	N/A			Unlikely bacteria source
13	Interstate 57	43.221744	-87.98	5900 W Mequon Rd, Mequon, WI 53092	Ozaukee	City of Mequon	Suburban	Restaurant	Google Maps	N/A			Unlikely bacteria source
14	Interstate 57	43.220560	-87.98	11120 N Cedarburg Rd, Mequon, WI 53092	Ozaukee	City of Mequon	Suburban	Restaurant	Google Maps	N/A			Unlikely bacteria source
15	Interstate 57	43.22	-87.98	6005 W Mequon Rd, Mequon, WI 53092	Ozaukee	City of Mequon	Suburban	Restaurant	Google Maps	N/A			Unlikely bacteria source
16	Interstate 57	43.21	-87.97	10365 N Cedarburg Rd, Mequon, WI 53092	Ozaukee	City of Mequon	Suburban	Restaurant	Google Maps	N/A			Unlikely bacteria source
17	Interstate 57	43.19	-87.97	4327 W Schroeder Dr, Brown Deer, WI 53223	Milwaukee	Village of Brown Deer	Suburban	Restaurant	Google Maps	N/A			Unlikely bacteria source
18	Interstate 57	43.18	-87.96	9133 N Deerwood Dr, Brown Deer, WI 53223	Milwaukee	Village of Brown Deer	Suburban	Restaurant	Google Maps	N/A			Unlikely bacteria source
19	US Highway 45	43.65	-88.26	N3104 US-45, Campbellsport, WI 53010	Fond du Lac	Town of Osceola	Rural	Restaurant	Google Maps	N/A			Unlikely bacteria source
20	US Highway 45	43.62	-88.24	N 2245 US-45, Campbellsport, WI 53010	Fond du Lac	Town of Auburn	Rural	Restaurant	Google Maps	N/A			Unlikely bacteria source
21	US Highway 45	43.60	-88.24	N1690 US-45, Campbellsport, WI 53010	Fond du Lac	Town of Auburn	Rural	Restaurant	Google Maps	N/A			Unlikely bacteria source
22	US Highway 45	43.55	-88.24	192 US-45, Kewaskum, WI 53040	Fond du Lac	Town of Auburn	Rural	Restaurant	Google Maps	N/A			Unlikely bacteria source

ID	Highway Name	Lat	Long	Address/Location	County	City/Village/Town	Rural/Urban/Suburban	Type of Bacteria Source	GIS Source (Google Earth, County/Municipal GIS, WisDOT DOTView)	Date of Field Verification	Google Street View Screen Shots	Google Aerial Map View	Decision
23	State Highway 60	43.32	-88.18	N168W21991 Main St, Jackson, WI 53037	Washington	Village of Jackson	Rural	Restaurant	Google Maps	N/A			Unlikely bacteria source
24	State Highway 60	43.32	-88.17	W213 N16770, Glen Brooke Dr, Jackson, WI 53037	Washington	Village of Jackson	Rural	Restaurant	Google Maps	N/A			Unlikely bacteria source
25	State Highway 60	43.32	-88.16	N168W20594 Main St, Jackson, WI 53037	Washington	Village of Jackson	Rural	Restaurant	Google Maps	N/A			Unlikely bacteria source
26	State Highway 60	43.32	-88.16	N168W20101 Main St, Jackson, WI 53037	Washington	Village of Jackson	Rural	Restaurant	Google Maps	N/A			Unlikely bacteria source
27	State Highway 60	43.32	-88.15	N168w19490 Main St, Jackson, WI 53037	Washington	Town of Jackson	Rural	Restaurant	Google Maps	N/A			Unlikely bacteria source
28	State Highway 100	43.08	-88.05	3131 N Mayfair Rd, Wauwatosa, WI 53222	Milwaukee	City of Wauwatosa	Urban	Restaurant	Google Maps	N/A			Unlikely bacteria source
29	State Highway 100	43.07	-88.05	3041 N Mayfair Rd, Wauwatosa, WI 53222	Milwaukee	City of Wauwatosa	Urban	Restaurant	Google Maps	N/A			Unlikely bacteria source
30	State Highway 100	43.07	-88.05	2751 N Mayfair Rd, Wauwatosa, WI 53222	Milwaukee	City of Wauwatosa	Urban	Restaurant	Google Maps	N/A			Unlikely bacteria source
31	State Highway 100	43.07	-88.05	2711 N Mayfair Rd, Wauwatosa, WI 53222	Milwaukee	City of Wauwatosa	Urban	Restaurant	Google Maps	N/A			Unlikely bacteria source
32	State Highway 100	43.07	-88.05	2635 N Mayfair Rd, Wauwatosa, WI 53226	Milwaukee	City of Wauwatosa	Urban	Restaurant	Google Maps	N/A			Unlikely bacteria source
33	State Highway 100	43.06	-88.05	2350 N Mayfair Rd, Wauwatosa, WI 53226	Milwaukee	City of Wauwatosa	Urban	Restaurant	Google Maps	N/A			Unlikely bacteria source
34	State Highway 100	43.06	-88.05	2201 N Mayfair Rd, Wauwatosa, WI 53226	Milwaukee	City of Wauwatosa	Urban	Restaurant	Google Maps	N/A			Unlikely bacteria source
35	State Highway 100	43.05	-88.05	1701 N Mayfair Rd, Wauwatosa, WI 53226	Milwaukee	City of Wauwatosa	Urban	Restaurant	Google Maps	N/A			Unlikely bacteria source
36	State Highway 100	43.04	-88.05	869 N Mayfair Rd, Wauwatosa, WI 53226	Milwaukee	City of Wauwatosa	Urban	Restaurant	Google Maps	N/A			Unlikely bacteria source
37	State Highway 100	43.04	-88.05	817 N Mayfair Rd, Milwaukee, WI 53226	Milwaukee	City of Wauwatosa	Urban	Restaurant	Google Maps	N/A			Unlikely bacteria source
38	State Highway 100	43.04	-88.05	725 N Mayfair Rd, Wauwatosa, WI 53226	Milwaukee	City of Wauwatosa	Urban	Restaurant	Google Maps	N/A			Unlikely bacteria source
39	State Highway 100	43.04	-88.05	510 N Mayfair Rd, Wauwatosa, WI 53226	Milwaukee	City of Wauwatosa	Urban	Restaurant	Google Maps	N/A			Unlikely bacteria source
40	State Highway 100	43.04	-88.05	418 N Mayfair Rd, Wauwatosa, WI 53226	Milwaukee	City of Wauwatosa	Urban	Restaurant	Google Maps	N/A			Unlikely bacteria source
41	State Highway 100	43.04	-88.05	10842 W Bluemound Rd, Wauwatosa, WI 53226	Milwaukee	City of Wauwatosa	Urban	Restaurant	Google Maps	N/A			Unlikely bacteria source
42	State Highway 100	43.03	-88.05	151 N Mayfair Rd, Wauwatosa, WI 53226	Milwaukee	City of Wauwatosa	Urban	Restaurant	Google Maps	N/A			Unlikely bacteria source
43	State Highway 100	43.02	-88.05	837 S 108th St, Milwaukee, WI 53214	Milwaukee	City of West Allis	Urban	Restaurant	Google Maps	N/A			Unlikely bacteria source
44	State Highway 100	43.02	-88.05	1117 S 108th St, Milwaukee, WI 53214	Milwaukee	City of West Allis	Urban	Restaurant	Google Maps	N/A			Unlikely bacteria source
45	State Highway 100	43.02	-88.05	1234 S 108th St, West Allis, WI 53214	Milwaukee	City of West Allis	Urban	Restaurant	Google Maps	N/A			Unlikely bacteria source

ID	Highway Name	Lat	Long	Address/Location	County	City/Village/Town	Rural/Urban/Suburban	Type of Bacteria Source	GIS Source (Google Earth, County/Municipal GIS, WisDOT DOTView)	Date of Field Verification	Google Street View Screen Shots	Google Aerial Map View	Decision
46	State Highway 100	43.02	-88.05	1427 S 108th St, West Allis, WI 53214	Milwaukee	City of West Allis	Urban	Restaurant	Google Maps	N/A			Unlikely bacteria source
47	State Highway 100	43.01	-88.05	1500 S 108th St, West Allis, WI 53214	Milwaukee	City of West Allis	Urban	Restaurant	Google Maps	N/A			Unlikely bacteria source
48	State Highway 100	43.01	-88.05	1507 S 108th St, West Allis, WI 53214	Milwaukee	City of West Allis	Urban	Restaurant	Google Maps	N/A			Unlikely bacteria source
49	State Highway 100	43.01	-88.05	1550 S 108th St, West Allis, WI 53214	Milwaukee	City of West Allis	Urban	Restaurant	Google Maps	N/A			Unlikely bacteria source
50	State Highway 100	43.01	-88.05	1657 S 108th St, West Allis, WI 53214	Milwaukee	City of West Allis	Urban	Restaurant	Google Maps	N/A			Unlikely bacteria source
51	State Highway 36	42.95	-87.98	5235 W Loomis Rd, Greendale, WI 53129	Milwaukee	City of Greenfield	Urban	Restaurant	Google Maps	N/A			Unlikely bacteria source
52	State Highway 36	42.95	-87.98	5110 W Loomis Rd, Greendale, WI 53129	Milwaukee	City of Greenfield	Urban	Restaurant	Google Maps	N/A			Unlikely bacteria source
53	State Highway 36	42.95	-87.98	5045 W Loomis Rd, Milwaukee, WI 53220	Milwaukee	City of Greenfield	Urban	Restaurant	Google Maps	N/A			Unlikely bacteria source
54	State Highway 36	42.96	-87.97	4306 W Layton Ave, Greenfield, WI 53220	Milwaukee	City of Greenfield	Urban	Restaurant	Google Maps	N/A			Unlikely bacteria source
55	State Highway 36	42.96	-87.97	4060 W Loomis Rd, Milwaukee, WI 53221	Milwaukee	City of Greenfield	Urban	Restaurant	Google Maps	N/A			Unlikely bacteria source
56	State Highway 36	42.97	-87.96	3400 W Loomis Rd, Milwaukee, WI 53221	Milwaukee	City of Greenfield	Urban	Restaurant	Google Maps	N/A			Unlikely bacteria source
57	State Highway 36	42.97	-87.96	3374 W Loomis Rd, Greenfield, WI 53221	Milwaukee	City of Greenfield	Urban	Vet	Google Maps	N/A			Unlikely bacteria source
58	State Highway 36	42.98	-87.96	3322 W Loomis Rd, Greenfield, WI 53221	Milwaukee	City of Greenfield	Urban	Restaurant	Google Maps	N/A			Unlikely bacteria source
59	State Highway 241	42.98	-87.95	3606 S 27th St, Milwaukee, WI 53221	Milwaukee	Town of Lake	Urban	Restaurant	Google Maps	N/A			Unlikely bacteria source
60	State Highway 241	42.98	-87.95	3657 S 27th St, Milwaukee, WI 53221	Milwaukee	Town of Lake	Urban	Restaurant	Google Maps	N/A			Unlikely bacteria source
61	State Highway 241	42.98	-87.95	3801 S 27th St, Milwaukee, WI 53221	Milwaukee	Town of Lake	Urban	Restaurant	Google Maps	N/A			Unlikely bacteria source
62	State Highway 241	42.97	-87.95	3871 S 27th St, Milwaukee, WI 53221	Milwaukee	Town of Lake	Urban	Restaurant	Google Maps	N/A			Unlikely bacteria source
63	State Highway 241	42.97	-87.95	4275 S 27th St, Greenfield, WI 53221	Milwaukee	City of Greenfield	Urban	Restaurant	Google Maps	N/A			Unlikely bacteria source
64	State Highway 241	42.96	-87.95	4624 S 27th St, Milwaukee, WI 53221	Milwaukee	City of Milwaukee	Urban	Restaurant	Google Maps	N/A			Unlikely bacteria source
65	State Highway 241	42.96	-87.95	4756 S 27th St, Milwaukee, WI 53221	Milwaukee	City of Milwaukee	Urban	Restaurant	Google Maps	N/A			Unlikely bacteria source
66	State Highway 241	42.96	-87.95	4841 S 27th St, Greenfield, WI 53221	Milwaukee	City of Greenfield	Urban	Restaurant	Google Maps	N/A			Unlikely bacteria source
67	State Highway 241	42.95	-87.95	5051 S 27th St, Greenfield, WI 53221	Milwaukee	City of Greenfield	Urban	Restaurant	Google Maps	N/A			Unlikely bacteria source
68	State Highway 241	42.95	-87.95	5077 S 27th St, Greenfield, WI 53221	Milwaukee	City of Greenfield	Urban	Restaurant	Google Maps	N/A			Unlikely bacteria source

ID	Highway Name	Lat	Long	Address/Location	County	City/Village/Town	Rural/Urban/Suburban	Type of Bacteria Source	GIS Source (Google Earth, County/Municipal GIS, WisDOT DOTView)	Date of Field Verification	Google Street View Screen Shots	Google Aerial Map View	Decision
69	US Highway 45	43.65	-88.26	-	Fond du Lac	Town of Osceola	Rural	Propan storage tank	Google Maps	N/A			Unlikely bacteria source
70	US Highway 45	43.60	-88.24	W1728 Sunset Dr, Campbellsport, WI 53010	Fond du Lac	Town of Auburn	Rural	Storage	Google Maps	N/A			Unlikely bacteria source
71	US Highway 45	43.59	-88.24	W1782 Co Rd Y, Campbellsport, WI 53010	Fond du Lac	Town of Auburn	Rural	Farm	Google Maps	N/A			Unlikely bacteria source
72	State Highway 60	43.32	-88.19	3160 WI-60 Trunk, Jackson, WI 53037	Washington	Town of Polk	Rural	Auto Body	Google Maps	N/A			Unlikely bacteria source
73	State Highway 60	43.32	-88.18	N168W22224 Main St, Jackson, WI 53037	Washington	Town of Jackson	Rural	Gas Station	Google Maps	N/A			Unlikely bacteria source
74	State Highway 60	43.32	-88.18	N168w22225 Main St, Jackson, WI 53037	Washington	Town of Jackson	Rural	Sanitary Sewer Overflow	Google Maps	N/A			Unlikely bacteria source
75	State Highway 60	43.32	-88.18	1 Pharmacal Way, Jackson, WI 53037	Washington	Town of Jackson	Rural	Sanitary Sewer Overflow	Google Maps	N/A			Unlikely bacteria source
76	State Highway 60	43.32	-88.18	-	Washington	Town of Jackson	Rural	Sanitary Sewer Overflow	Google Maps	N/A			Unlikely bacteria source
77	State Highway 60	43.32	-88.17	-	Washington	Town of Jackson	Rural	Sanitary Sewer Overflow	Google Maps	N/A			Unlikely bacteria source
78	State Highway 60	43.323645,	-88.15	W194N16774 Eagle Dr, Jackson, WI 53037	Washington	Town of Jackson	Rural	Sanitary Sewer Overflow	Google Maps	N/A			Unlikely bacteria source
79	Interstate 43	43.41	-87.91	-	Ozaukee	Town of Port Washington	Rural	Sanitary Sewer Overflow	Google Maps	N/A			Unlikely bacteria source
80	Interstate 43	43.41	-87.91	-	Ozaukee	Town of Port Washington	Rural	Sanitary Sewer Overflow	Google Maps	N/A			Unlikely bacteria source
81	Interstate 43	43.41	-87.91	-	Ozaukee	Town of Port Washington	Rural	Sanitary Sewer Overflow	Google Maps	N/A			Unlikely bacteria source
82	Interstate 43	43.35	-87.93	-	Ozaukee	Town of Grafton	Rural	Sanitary Sewer Overflow	Google Maps	N/A			Unlikely bacteria source

ID	Highway Name	Lat	Long	Address/Location	County	City/Village/Town	Rural/Urban/Suburban	Type of Bacteria Source	GIS Source (Google Earth, County/Municipal GIS, WisDOT DOTView)	Date of Field Verification	Google Street View Screen Shots	Google Aerial Map View	Decision
83	Interstate 43	43.33	-87.92	-	Ozaukee	Town of Grafton	Rural	Sanitary Sewer Overflow	Google Maps	N/A			Unlikely bacteria source
84	Interstate 43	43.33	-87.92	-	Ozaukee	Town of Grafton	Rural	Sanitary Sewer Overflow	Google Maps	N/A			Unlikely bacteria source
85	Interstate 43	43.32	-87.92	-	Ozaukee	Town of Grafton	Rural	Sanitary Sewer Overflow	Google Maps	N/A			Unlikely bacteria source
86	Interstate 43	43.31	-87.92	-	Ozaukee	Town of Grafton	Rural	Sanitary Sewer Overflow	Google Maps	N/A			Unlikely bacteria source
87	Interstate 43	43.31	-87.92	-	Ozaukee	Town of Grafton	Rural	Sanitary Sewer Overflow	Google Maps	N/A			Unlikely bacteria source

Appendix 2c - Possible Bacteria Sites Included in the Field Investigation

Field Investigation ID	Field Investigation Location	Highway Name	Lat	Long	Address/Location	County	City/Village/Town	Rural/Urban/Suburban	Type of Bacteria Source	GIS Source (Google Earth, County/Municipal GIS, WisDOT DOTView)	Date of Field Verification	Google Street View Screen Shots	Notes	Google Aerial Map View	Decision
1	US 45-1	US Highway 45	43.67	-88.27	N3832 County Rd W, Eden, WI 53019	Fond du Lac	Town of Osceola	Rural	CAFO	Google Maps	10/8/2024		Appears standing water could be runoff from the CAFO		Bacteria Source
2	US 45-2	US Highway 45	43.60	-88.24	N1776 US-45, Campbellsport, WI 53010	Fond du Lac	Town of Auburn	Rural	Truck repair	Google Maps	10/8/2024		Appears standing water could be runoff from the truck repair shop		Bacteria Source
3	STH 67-3	State Highway 67	43.60	-88.29	W2796 WI-67, Campbellsport, WI 53010	Fond du Lac	Village of Campbellsport	Suburban	Veterinary Clinic	Google Maps	10/8/2024		Appears standing water could be runoff from the vet clinic		Bacteria Source
4	US 45-4	US Highway 45	43.51	-88.22	8715 Fond Du Lac Ave, Kewaskum, WI 53040	Washington and Fond du Lac	Village of Kewaskum	Suburban	Farm	Google Maps	10/8/2024		Potential runoff to ROW from the small farm		Bacteria Source
5	US 45-5	US Highway 45	43.50	-88.22	500 Trading Post Ln, Kewaskum, WI 53040	Washington and Fond du Lac	Village of Kewaskum	Suburban	Pet Store	Google Maps	10/8/2024		Appears standing water could be runoff from the pet store		Bacteria Source
6	STH 60-6	State Highway 60	43.32	-88.24	2015 American Eagle Dr, Slinger, WI 53086	Washington	Village of Slinger	Rural	Veterinary Clinic	Google Maps	10/8/2024		Drainage from vet clinic appears to drain towards ROW		Bacteria Source
7	STH 60-7	State Highway 60	43.32	-88.21	3473 WI-60 Trunk, Jackson, WI 53037	Washington	Town of Polk	Rural	Pet Resort	Google Maps	10/8/2024		Appears standing water could be runoff from the pet resort		Bacteria Source
8	STH 60-8	State Highway 60	43.32	-88.18	1 Pharmacal Way, Jackson, WI 53037	Washington	Town of Jackson	Rural	Restaurant	Google Maps	10/8/2024		Appears standing water could be runoff from the pharmaceutical company		Bacteria Source

Appendix 3: Completed Investigation Forms

WisDOT Milwaukee River TMDL Study
Possible Bacteria Source Field Investigation Review Form

Part A: Possible Bacteria Source Site Information

City/Village/Town: Town of Osceola	County: Fond du Lac	WisDOT Highway Name: US 45	
Site Name: 2nd Look Holsteins (CAFO)		Address: N3832 County Rd W, Eden, WI 53019	
Field Investigation Location*: US 45-1		Review date: 10/8/24	Weather: 59°F Sunny
Reviewers: Lu Chen/ Emma Connell/ Rachel Clark/ John Voorhees/ Hans Hallanger		Arrived: 11:19am	Left: 11:32am
Reviewer Firm/Region: AECOM		Site Owner: Private	
Possible Source Type: This site is a Confined Animal Feeding Operation (CAFO)			

**Reference Location Map*



Source Location: LAT: 43.674702°, LNGT: -88.273318°

○ Inspection areas

Part B: Field Investigation Results

Field Investigation Questions	Comments
Investigation Location?	US 45 roadside ditch drainage ditch adjacent to the intersection of US45 & CTHW that receives runoff from the CAFO
Field Evidence of Bacteria?	No

ADDITIONAL COMMENTS:

- This site is a CAFO.
- Drainage path uncertain once it gets to USH 45

Photos:



Likely drainage path from CAFO site to highway



Another possible drainage path through the field

WisDOT Milwaukee River TMDL Study
Possible Bacteria Source Field Investigation Review Form

Part A: Possible Bacteria Source Site Information

City/Village/Town: Town of Auburn	County: Fond du Lac	WisDOT Highway Name: US 45
Site Name: OHenry's Service	Address: N1776 US-45, Campbellsport, WI 53010	
Field Investigation Location*: US 45-2	Review date: 10/8/24	Weather: 61°F Sunny
Reviewers: Lu Chen/ Emma Connell/ Rachel Clark/ John Voorhees/ Hans Hallanger	Arrived: 11:59am	Left: 12:10pm
Reviewer Firm/Region: AECOM	Site Owner: Private (OHenry's Service)	
Possible Source Type: This site is an auto repair shop		

**Reference Location Map*



Source Location: LAT: 43.604034°, LNGT: -88.24069°

○ Inspection areas

Part B: Field Investigation Results

Field Investigation Questions	Comments
Investigation Location?	US 45 roadside ditches that receives drainage from the O'Henry's Service flows eastward and collects in a natural low area
Field Evidence of Bacteria?	No

ADDITIONAL COMMENTS: None

Photos:





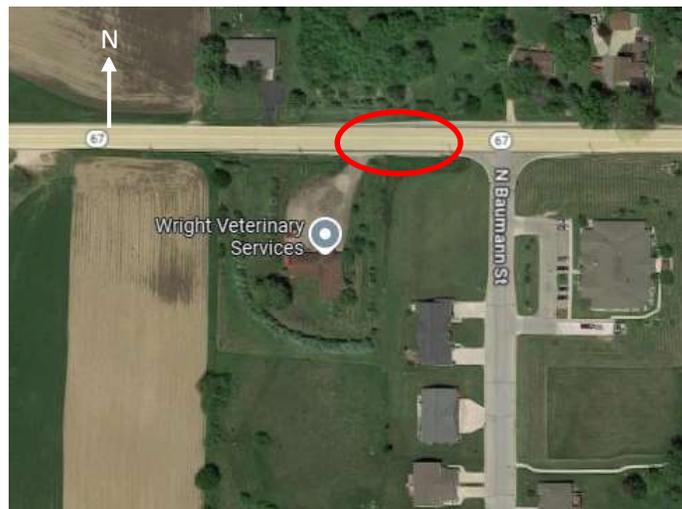


WisDOT Milwaukee River TMDL Study
Possible Bacteria Source Field Investigation Review Form

Part A: Possible Bacteria Source Site Information

City/Village/Town: Village of Campbellsport	County: Fond du Lac	WisDOT Highway Name: STH 67
Site Name: Wright Veterinary Services	Address: W2796 WI-67, Campbellsport, WI 53010	
Field Investigation Location*: STH 67-3	Review date: 10/8/24	Weather: 59°F Sunny
Reviewers: Lu Chen/ Emma Connell/ Rachel Clark/ John Voorhees/ Hans Hallanger	Arrived: 11:44am	Left: 11:47am
Reviewer Firm/Region: AECOM	Site Owner: Private (Wright Veterinary Services)	
Possible Source Type: This site is a veterinary clinic		

**Reference Location Map*



Source Location: LAT: 43.601115°, LNGT: -88.294385°

○ Inspection areas

Part B: Field Investigation Results

Field Investigation Questions	Comments
Investigation Location?	STH 67 ROW area that receives drainage from Wright Veterinary Clinic flows eastward into a vegetated buffer area before passing beneath STH 67 to the north
Field Evidence of Bacteria?	No

ADDITIONAL COMMENTS: None

Photos:



Drainage towards vegetated buffered area



Covered dumpsters

WisDOT Milwaukee River TMDL Study
Possible Bacteria Source Field Investigation Review Form

Part A: Possible Bacteria Source Site Information

City/Village/Town: Village of Kewaskum	County: Washington and Fond du Lac	WisDOT Highway Name: US 45
Site Name: Unnamed farm	Address: 8715 Fond Du Lac Ave, Kewaskum, WI 53040	
Field Investigation Location*: US 45-4	Review date: 10/8/24	Weather: 62°F Sunny
Reviewers: Lu Chen/ Emma Connell/ Rachel Clark/ John Voorhees/ Hans Hallanger	Arrived: 12:26pm	Left: 12:30pm
Reviewer Firm/Region: AECOM	Site Owner: Private (Unnamed farm)	
Possible Source Type: This site is a farm		

**Reference Location Map*



Source Location: LAT: 43.508148°, LNGT: -88.219337°

○ Inspection areas

Part B: Field Investigation Results

Field Investigation Questions	Comments
Investigation Location?	US 45 roadside ditch that collects drainage from a farmyard near US 45. The manure is in an enclosed barnyard located outside of the ROW.
Field Evidence of Bacteria?	No

ADDITIONAL COMMENTS: None

Photos:



Drainage towards vegetated ditch

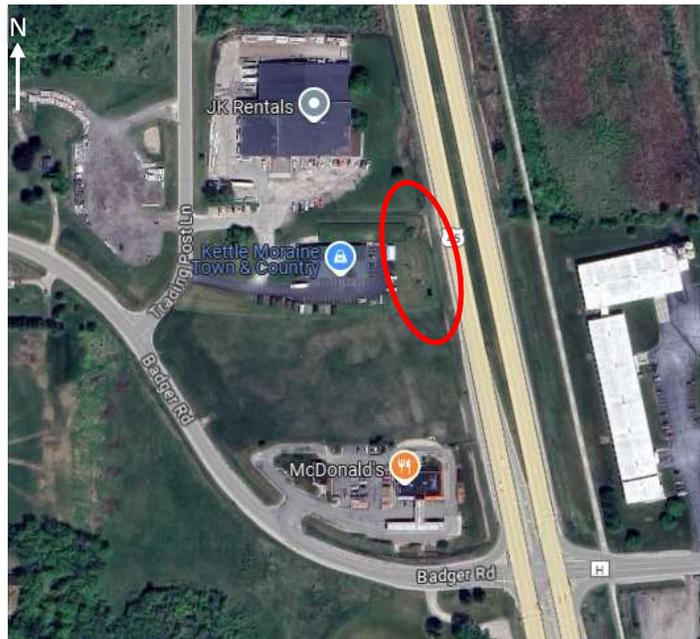


WisDOT Milwaukee River TMDL Study
Possible Bacteria Source Field Investigation Review Form

Part A: Possible Bacteria Source Site Information

City/Village/Town: Village of Kewaskum		County: Washington and Fond du Lac	WisDOT Highway Name: US 45
Site Name: Kettle Moraine Town & Country		Address: 500 Trading Post Ln, Kewaskum, WI 53040	
Field Investigation Location*: US 45-5		Review date: 10/8/24	Weather: 62°F Sunny
Reviewers: Lu Chen/ Emma Connell/ Rachel Clark/ John Voorhees/ Hans Hallanger		Arrived: 12:36pm	Left: 12:47pm
Reviewer Firm/Region: AECOM		Site Owner: Private (Kettle Moraine Town & Country)	
Possible Source Type: This site is a pet store			

**Reference Location Map*



Source Location: LAT: 43.498641°, LNGT: -88.216362°

○ Inspection areas

Part B: Field Investigation Results

Field Investigation Questions	Comments
Investigation Location?	US 45 roadside ditch that receives drainage from Kettle Moraine Town & Country flows toward a wide grass buffer and into a drainage swale, with a well-maintained grassy pet area nearby and dense grass along the fence.
Field Evidence of Bacteria?	No

ADDITIONAL COMMENTS: None

Photos:



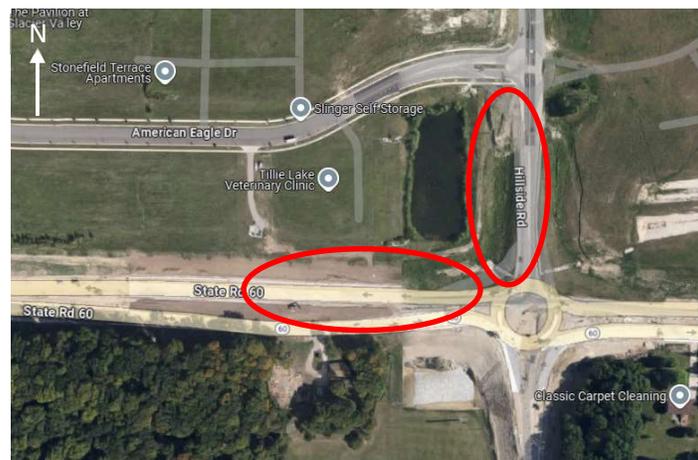


WisDOT Milwaukee River TMDL Study
Possible Bacteria Source Field Investigation Review Form

Part A: Possible Bacteria Source Site Information

City/Village/Town: Village of Slinger	County: Washington	WisDOT Highway Name: STH 60
Site Name: Tilie Lake Veterinary Clinic	Address: 2015 American Eagle Dr, Slinger, WI 53086	
Field Investigation Location*: STH 60-6	Review date: 10/8/24	Weather: 53°F Sunny
Reviewers: Lu Chen/ Emma Connell/ Rachel Clark/ John Voorhees/ Hans Hallanger	Arrived: 9:50am	Left: 10:00am
Reviewer Firm/Region: AECOM	Site Owner: Private (Tilie Lake Veterinary Clinic)	
Possible Source Type: This site is a veterinary clinic		

**Reference Location Map*



Source Location: LAT: 43.324507°, LNGT: -88.243066°

○ Inspection areas

Part B: Field Investigation Results

Field Investigation Questions	Comments
Investigation Location?	STH 60 roadside ditch west of the detention pond west of Tilie Lake Veterinary Clinic receives drainage from the clinic, while a vegetated buffer to the north collects runoff from the southern area.
Field Evidence of Bacteria?	No

ADDITIONAL COMMENTS: None

Photos:





Covered dumpsters



Fenced areas

WisDOT Milwaukee River TMDL Study
Possible Bacteria Source Field Investigation Review Form

Part A: Possible Bacteria Source Site Information

City/Village/Town: Town of Polk	County: Washington	WisDOT Highway Name: STH 60
Site Name: Cedar Creek Pet Resort	Address: 3473 WI-60 Trunk, Jackson, WI 53037	
Field Investigation Location*: STH 60-7	Review date: 10/8/24	Weather: 55°F Sunny
Reviewers: Lu Chen/ Emma Connell/ Rachel Clark/ John Voorhees/ Hans Hallanger	Arrived: 10:08am	Left: 10:15am
Reviewer Firm/Region: AECOM	Site Owner: Private (Cedar Creek Pet Resort)	
Possible Source Type: This site is a pet resort		

**Reference Location Map*



Source Location: LAT: 43.324392°, LNGT: -88.210159°

○ Inspection areas

Part B: Field Investigation Results

Field Investigation Questions	Comments
Investigation Location?	STH 60 vegetated buffer area north of Cedar Creek Pet Resort receives drainage from the facility before it enters STH 60 ROW.
Field Evidence of Bacteria?	No

ADDITIONAL COMMENTS: None

Photos:



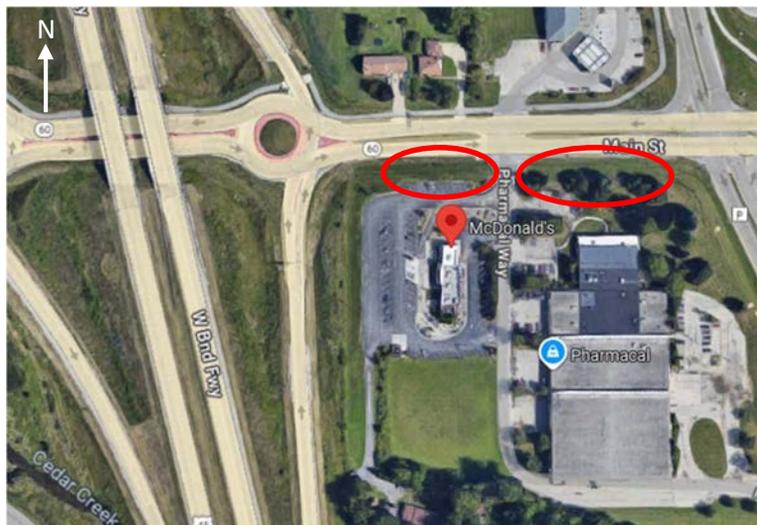


WisDOT Milwaukee River TMDL Study
Possible Bacteria Source Field Investigation Review Form

Part A: Possible Bacteria Source Site Information

City/Village/Town: Town of Jackson	County: Washington	WisDOT Highway Name: STH 60
Description: McDonald's & Pharmacial	Address: 1 Pharmacial Way, Jackson, WI 53037	
Field Investigation Location*: STH 60-8	Review date: 10/8/24	Weather: 55°F Sunny
Reviewers: Lu Chen/ Emma Connell/ Rachel Clark/ John Voorhees/ Hans Hallanger	Arrive: 10:21am	Left: 10:35am
Reviewer Firm/Region: AECOM	Site Owner: Private (McDonald's & Pharmacial)	
Possible Source Type: Pharmaceutical manufacturer and fast-food restaurant located off STH 60.		

**Reference Location Map*



Source Location: LAT: 43.324341°, LNGT: -88.184697°

○ Inspection areas

Part B: Field Investigation Results

Assessment	Comments
Investigation Location?	STH 60 roadside ditch drainage ditch off the STH 60 that receives runoff from the McDonald's and Pharmacial.
Field Evidence of Bacteria?	No

ADDITIONAL COMMENTS: None

Photos:







Section L – Additional DOT Developed Measurable Goals

Additional DOT Developed Measurable Goals

TS4 Permit Section	Program	Goal Number	Additional Measurable Goals	Tentative Schedule
2.1.6	Public Education & Outreach	1	Update Stormwater Management webpage to provide information on the DOT's winter salt reduction strategies.	Complete by End of Permit Term
		2	Update Stormwater Management webpage to provide information on what the public can do to reduce the extent of their salt use.	Complete by End of Permit Term
		3	Develop a schedule and plan to provide stormwater related messages and education to in-house staff.	Begin by End of Permit Term
		4	Identify opportunities and develop a schedule to provide stormwater-related messages and education to the public through the department's social media accounts.	Begin by End of Permit Term
		5	Regional SWECEs will attend a minimum of 3 stormwater-related trainings over the course of the permit term.	Complete/Ongoing
2.2.9	Public Involvement & Participation	1	Develop a Pilot Project, if feasible, in one county to provide litter pickup (mapping safe locations, accessible, recommended locations, adding to the inventory of locations, social media posts, etc.) at SCPs as part of the Adopt-A-Highway program.	Begin by End of Permit Term
		2	Provide the Stormwater Brochure at appropriate public events that the DOT has a Presence (Wisconsin State Fair for example). Discuss possibilities with Office of Public Affairs (OPA).	Complete by End of Permit Term
		3	Maintain a publicly accessible program email address and establish a monitoring schedule and process for responding to public inquiries.	Begin by End of Permit Term
2.3.7	Illicit Discharge Detection & Elimination	1	Establish an internal working group of staff responsible for addressing illicit discharges to improve statewide communication and coordination, determine education needs and establish protocols.	Begin by End of Permit Term
2.4.4	Construction Site Pollutant Control	1	Finalize the draft TRANS 401 rule and prepare for legislative review.	
		2	Update the department's policy manuals for the erosion control elements in the updated Trans 401.	Complete by End of Permit Term
		3	Provide erosion control training to region and consultant staff in at least three locations annually.	Complete/Ongoing
		4	Develop a process to track erosion control trainings taken by WisDOT engineers (in addition to the SWECEs).	Complete by End of Permit Term
		5	Develop annual erosion control trainings that focus on construction, design and permit compliance and provide training in at least 3 regions annually.	Complete/Ongoing
2.5.5	Post-Construction Stormwater Management	1	Inspect all new SCPs installed within 2 years of installation.	Complete/Ongoing
		2	Re-inspect all existing SCPs, by priority, to ensure inspection every 5 years.	Beginning in 2026
		3	Measure sediment depth at approximately 40 wet ponds per year until all wet ponds have been measured. Evaluate extent of re-inspections after all wet ponds are measured.	Beginning in 2026
		4	Update the department's policy manuals for the post-construction elements included in the updated Trans 401.	Complete by End of Permit Term
		5	Finalize the Stormwater Management Plan (SWMP) that is in development.	Complete by End of Permit Term
		6	Update the FDM to provide guidance on the use of the SWMP.	Complete by End of Permit Term
		7	Provide training/education and outreach on the SWMP.	Complete by End of Permit Term
		8	Monitor research being conducted by others on the use of Biochar as a soil amendment, and explore the applicability for use on existing and new SCPs.	Ongoing
		9	Research vegetation management practices as a potential phosphorus treatment option near SCPs.	Ongoing
2.6.5	Pollution Prevention	1	Implement SWPPP wet weather quarterly inspections utilizing consultant support.	Beginning in 2025
		2	Explore the potential of adding small SCPs or other treatment methods at SWPPP sites instead of providing for quarterly wet weather monitoring.	Begin by End of Permit Term
		3	Explore the potential of using electronic quarterly wet weather monitoring devices.	Begin by End of Permit Term