

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
3997-01-74	WISC 2023404	1

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

CITY OF WATERTOWN, CADY STREET ROCK RIVER BRIDGE P-28-0706 LOC STR JEFFERSON COUNTY

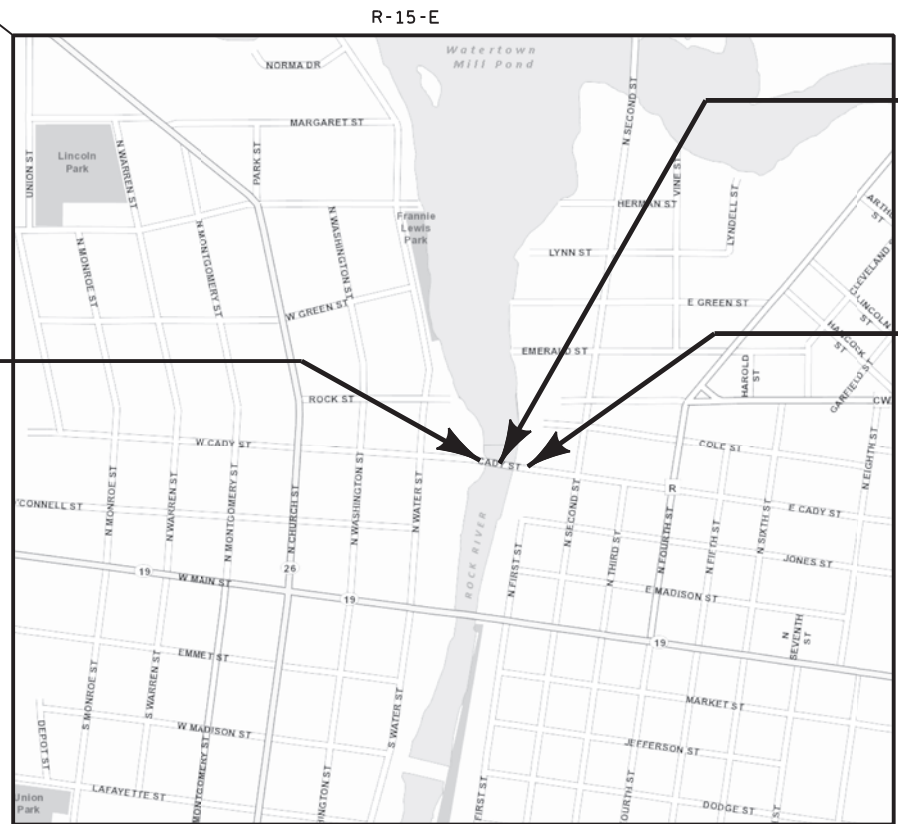
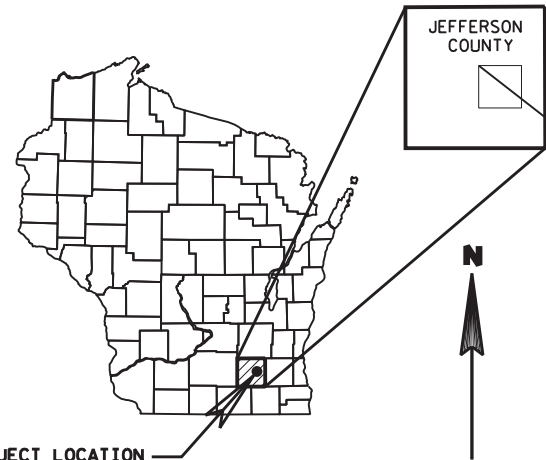
STATE PROJECT NUMBER
3997-01-74

PROJECT ID: 3997-01-74
WITH: N/A

ORDER OF SHEETS

Section No. 1	Title
Section No. 2	Typical Sections and Details (Includes Erosion Control Plans)
Section No. 3	Estimate of Quantities
Section No. 3	Miscellaneous Quantities
Section No. 4	Right of Way Plan
Section No. 5	Plan and Profile
Section No. 6	Standard Detail Drawings
Section No. 7	Sign Plates
Section No. 8	Structure Plans
Section No. 9	Computer Earthwork Data
Section No. 9	Cross Sections

TOTAL SHEETS = 30



BEGIN PROJECT
STA. 18+88.58
 Y = 438758.31
 X = 2308571.33

STRUCTURE P-28-0706

END PROJECT
STA. 21+11.42
 Y = 438720.19
 X = 2308790.84

DESIGN DESIGNATION

A.A.D.T. (2023)	=	4,200
A.A.D.T. (2043)	=	4,200
D.H.V.	=	420
D.	=	50/50
T.	=	5%
DESIGN SPEED	=	30 MPH
ESALS	=	N/A

CONVENTIONAL SYMBOLS PLAN

CORPORATE LIMITS	
PROPERTY LINE	
LOT LINE	
LIMITED HIGHWAY EASEMENT	
EXISTING RIGHT OF WAY	
PROPOSED OR NEW R/W LINE	
SLOPE INTERCEPT	
REFERENCE LINE	
EXISTING CULVERT	
PROPOSED CULVERT (Box or Pipe)	
COMBUSTIBLE FLUIDS	
HIGH VOLTAGE	
MARSH AREA	
WOODED OR SHRUB AREA	

PROFILE

GRADE LINE	
ORIGINAL GROUND	
MARSH OR ROCK PROFILE (To be noted as such)	
SPECIAL DITCH	
GRADE ELEVATION	
CULVERT (Profile View)	

UTILITIES

OVERHEAD	
ELECTRIC	
FIBER OPTIC	
GAS	
SANITARY SEWER	
STORM SEWER	
TELEPHONE	
WATER	
UTILITY PEDESTAL	
POWER POLE	
TELEPHONE POLE	



TOTAL NET LENGTH OF CENTERLINE = 0.042 MI.

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS), JEFFERSON COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES. ELEVATIONS ARE REFERENCED TO NAVD88 (2012). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 12A.

ACCEPTED FOR

City of Watertown

Date: 1/26/2023

Jaymellen G. Hillaway
City Engineer

ORIGINAL PLANS PREPARED BY

AYRES 3433 Oakwood Hills Parkway
Eau Claire, WI 54701
www.AyresAssociates.com

WISCONSIN
 KRISTOFER R. OLSON
 E-35236
 APPLETON, WI
 PROFESSIONAL ENGINEER

Kristofer R. Olson

DATE: 1/26/2023

**STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION**

PREPARED BY

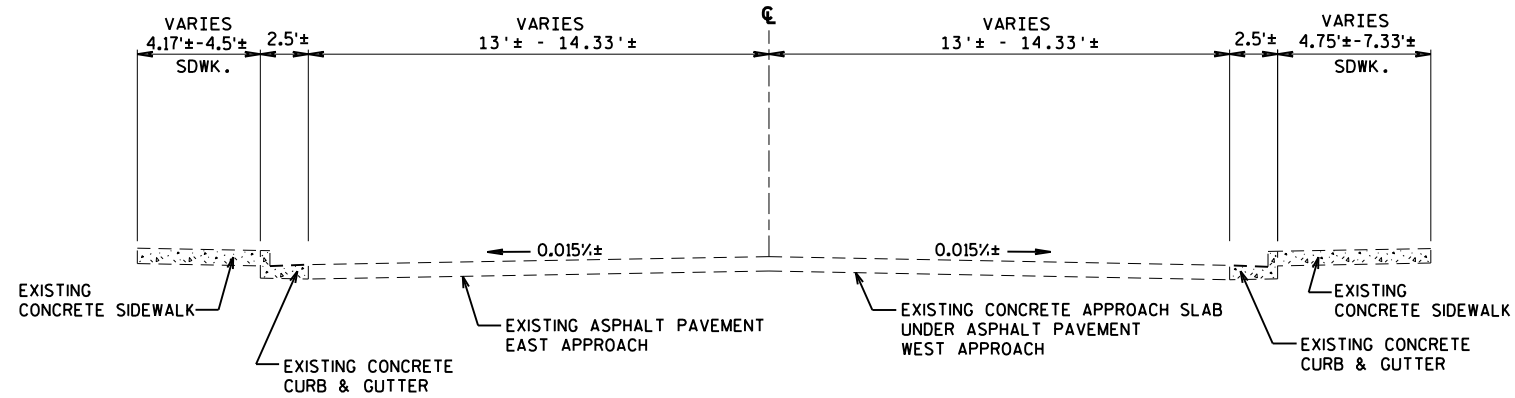
Surveyor	AYRES ASSOCIATES INC
Designer	AYRES ASSOCIATES INC
Project Manager	DELLA KOENIG, PE
Regional Examiner	
Regional Supervisor	KYLE HEMP, PE

APPROVED FOR THE DEPARTMENT

DATE: Jan 28, 2023

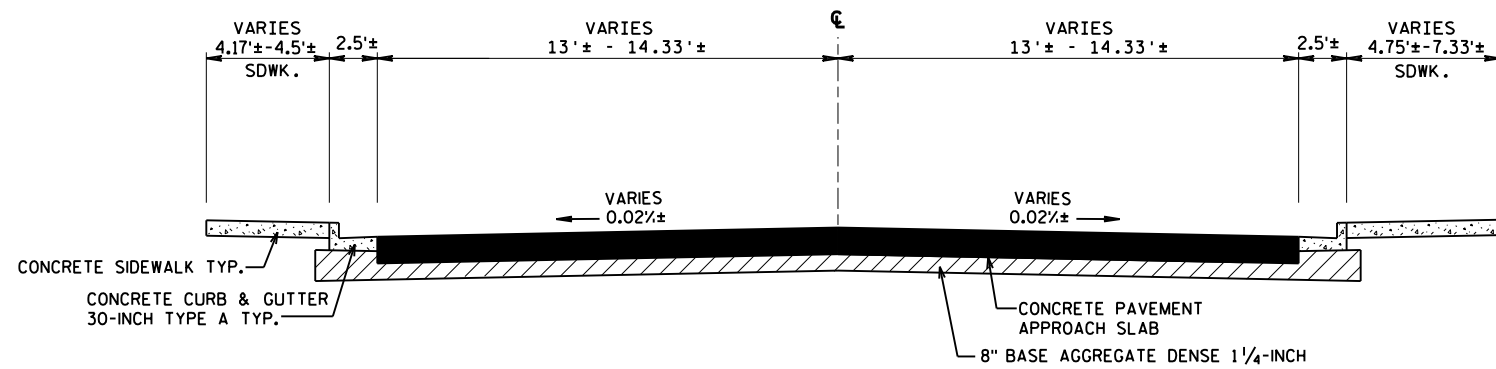
Della Koening
(Signature)

E



TYPICAL EXISTING SECTION

(CADY STREET)



FINISHED TYPICAL SECTION

(CADY STREET)

GENERAL NOTES

EROSION CONTROL ITEMS TO BE PLACED AS SHOWN ON THE PLAN OR AS DIRECTED BY THE ENGINEER.

NO TREES AND/OR SHRUBS ARE TO BE REMOVED WITHOUT THE APPROVAL OF THE ENGINEER.

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

ELEVATIONS SHOWN ON THIS PLAN ARE REFERENCED TO THE NORTH AMERICAN VERTICAL DATUM (NAVD 88).

THE LOCATION AND WIDTH OF THE EXISTING RIGHT OF WAY WAS DETERMINED BY THE MUNICIPALITY FOR THIS PROJECT. AYRES ASSOCIATES DOES NOT WARRANT IT'S ACCURACY.

ASPHALTIC SURFACE QUANTITIES WERE CALCULATED USING 112 LB/SY/IN.

UTILITIES

AT&T
220 WISCONSIN AVE.
WAUKESHA, WI 53186
ATTN: MATT DINNAUER
262-237-0042
md9542@att.com

WE ENERGIES
(ELECTRIC)
500 S 116TH STREET
WEST ALLIS, WI 53214
ATTN: ERIC KICKHAVER
414-944-5917
414-588-7472 (mobile)
Eric.Kickhaver@we-energies.com
(GAS)
700 KANE STREET
BURLINGTON, WI 53105
ATTN: SCOTT HOLSTEIN
262-763-1084
262-949-0490 (mobile)
scott.holstein@we-energies.com

WATERTOWN WATER UTILITY
800 HOFFMAN DRIVE
WATERTOWN, WI 53094
ATTN: PETER HARTZ
920-262-4075
PHartzr@cityofwatertown.org

SPECTRUM MID-AMERICA
N3760 COUNTY ROAD DJ
JUNEAU, WI 53039
ATTN: RONALD FRASE
608-438-9648
Ronald.Frase@charter.com

WATERTOWN COLLABORATIVE FIBER OPTIC NETWORK
106 JONES STREET
WATERTOWN, WI 53094
ATTN: ROBERT KAMINSKI (POLICE CHIEF)
920-262-4000
rkaminski@cityofwatertown.org

WISCONSIN DEPARTMENT OF NATURAL RESOURCES CONTACT:

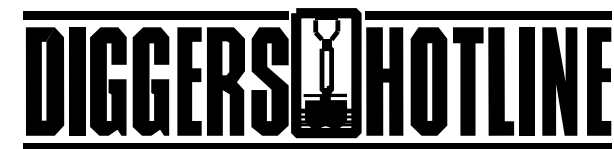
SHELLEY NELSON
3911 FISH HATCHERY ROAD
FITCHBURG, WI 53711
608-444-2835
shelley.nelson@wisconsin.gov

DESIGNER

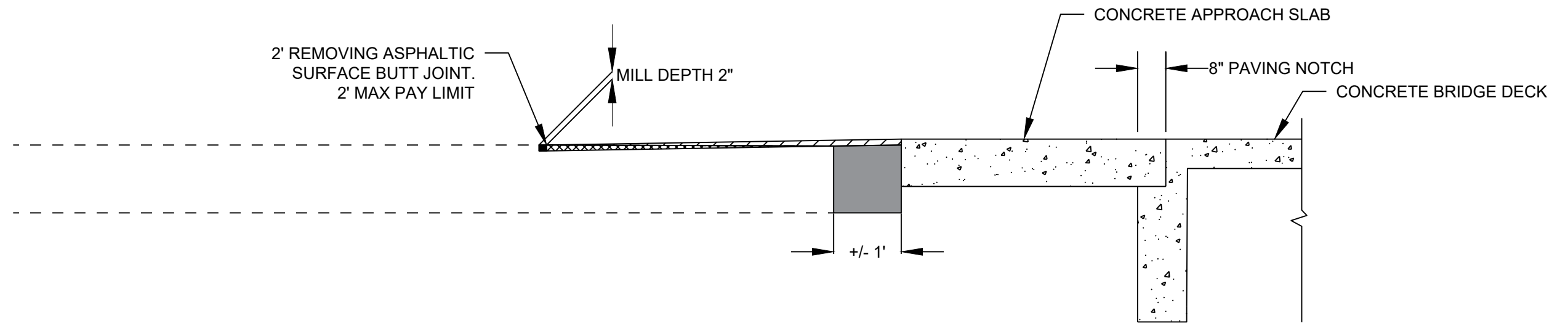
AYRES ASSOCIATES
3376 PACKERLAND DRIVE
ASHWAUBENON, WI 54115
ATTN: KRIS OLSON
920-498-1200
OlsonK@AyresAssociates.com




COUNTY CONTACT:

CITY OF WATERTOWN, ASSISTANT CITY ENGINEER
106 JONES STREET
WATERTOWN, WI 53094
ATTN: ANDREW BEYER
920-262-4052
AndrewB@cityofwatertown.org







Dial 811 or (800) 242-8511
www.DiggersHotline.com



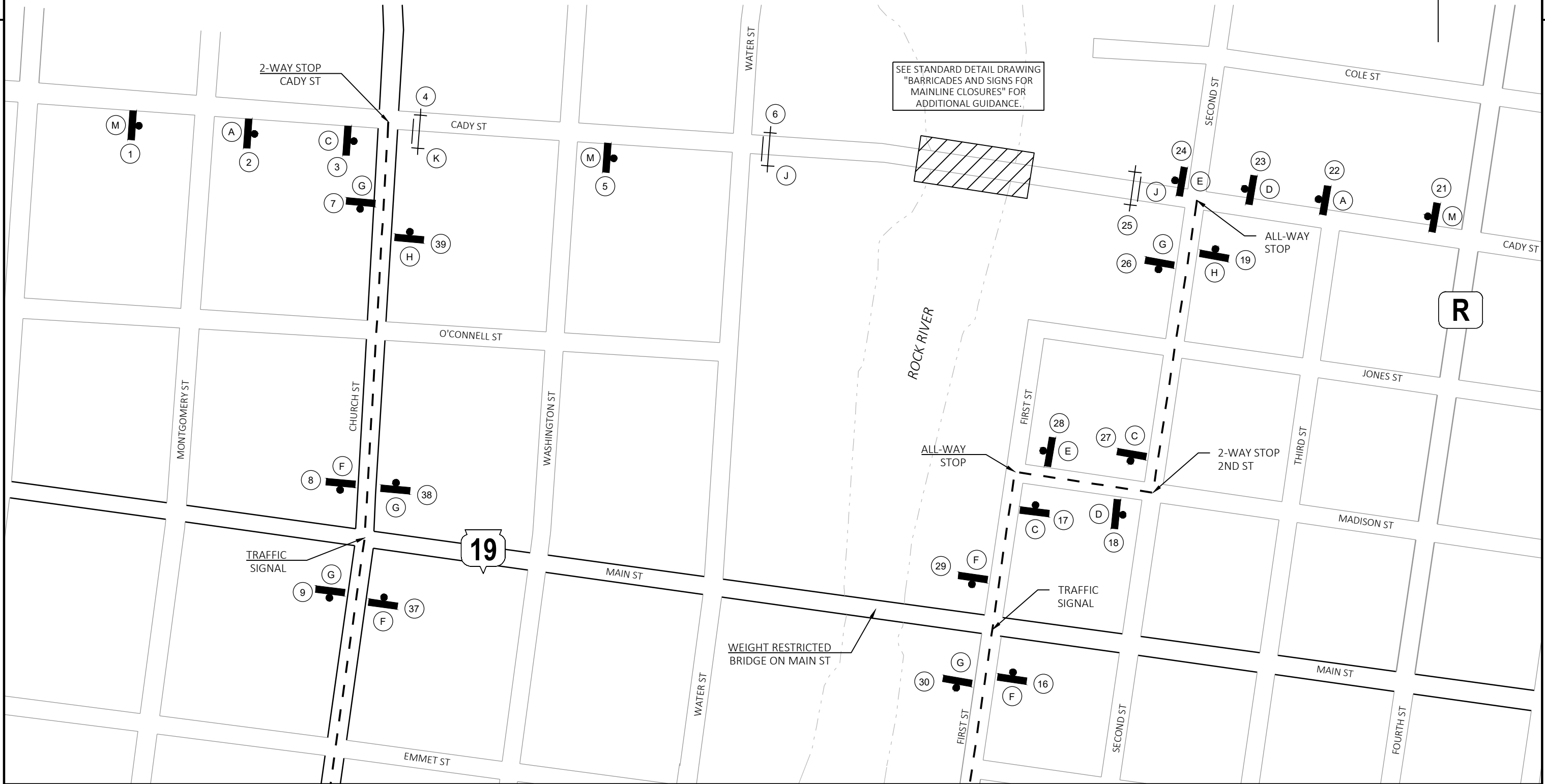
-  REMOVING ASPHALTIC SURFACE MILLING
-  PAVEMENT WEDGE (PAID AS ASPHALTIC SURFACE)
(MINIMUM LAYER THICKNESS 2.25" GRADATION 3, 1.75" GRADATION 4)
(MAXIMUM LAYER THICKNESS 3.0" GRADATION 3, 2.5" GRADATION 4)
-  TYPICAL 2" LAYER THICKNESS SURFACE (PAID AS ASPHALTIC SURFACE)

ASPHALTIC SURFACE MILLING AND PAVEMENT WEDGE DETAIL

LEGEND

-  DETOUR ROUTE
-  WORK ZONE AREA
-  TYPE III BARRICADE WITH SIGN
-  TRAFFIC CONTROL DETOUR SIGN

SEE NEXT SHEET FOR SIGN LEGEND



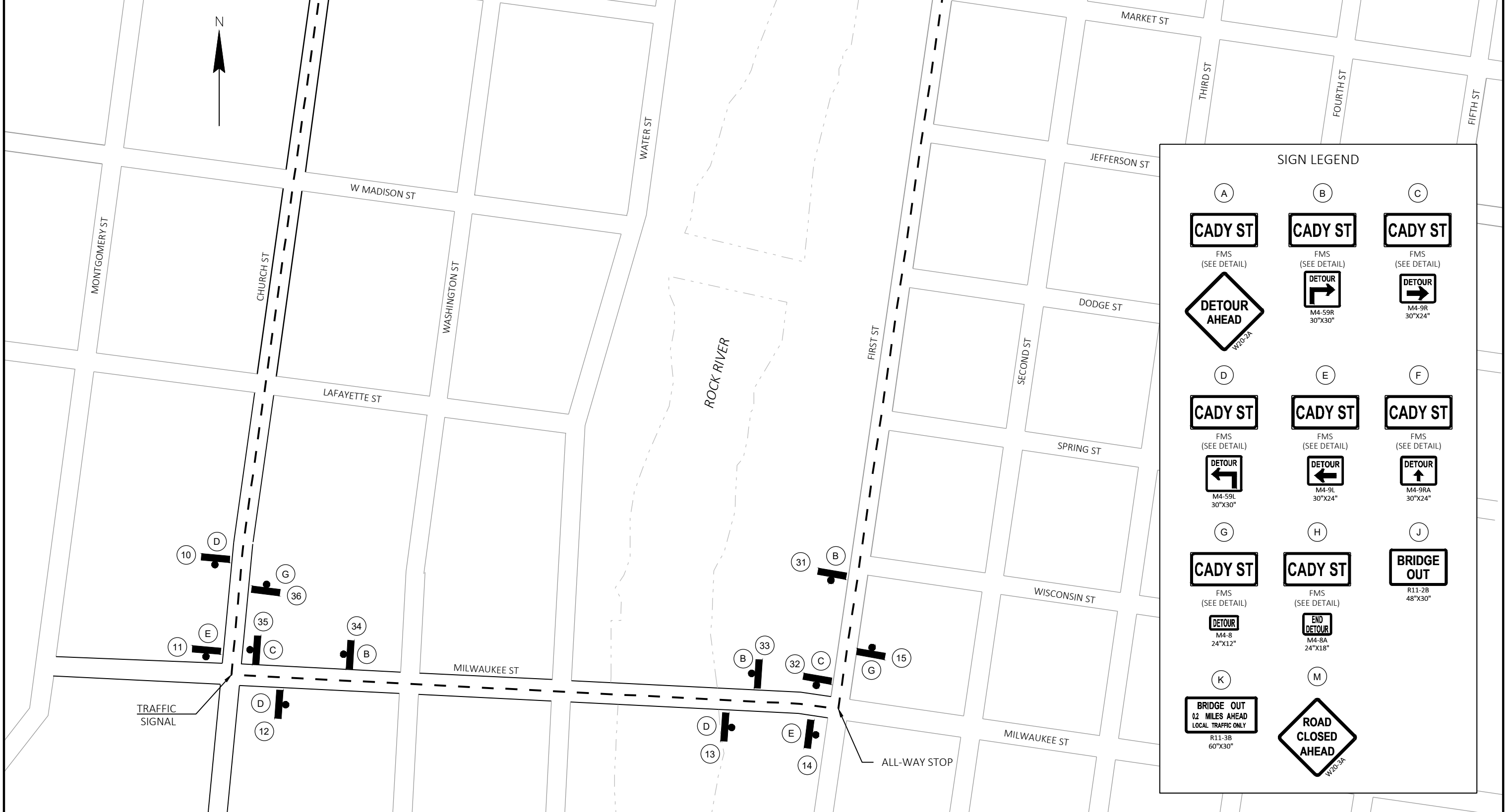
MATCHLINE

PROJECT NO: 3997-01-74	HWY: CADY STREET	COUNTY: JEFFERSON	DETOUR	SHEET	E
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LEGEND

- DETOUR ROUTE
- ▨ WORK ZONE AREA
- ⊥ TYPE III BARRICADE WITH SIGN
- ⬇ TRAFFIC CONTROL DETOUR SIGN


MATCHLINE



SIGN LEGEND

(A) CADY ST FMS (SEE DETAIL)	(B) CADY ST FMS (SEE DETAIL)	(C) CADY ST FMS (SEE DETAIL)
DETOUR AHEAD W20-2A	DETOUR M4-59R 30"x30"	DETOUR M4-9R 30"x24"
(D) CADY ST FMS (SEE DETAIL)	(E) CADY ST FMS (SEE DETAIL)	(F) CADY ST FMS (SEE DETAIL)
DETOUR M4-59L 30"x30"	DETOUR M4-9L 30"x24"	DETOUR M4-9RA 30"x24"
(G) CADY ST FMS (SEE DETAIL)	(H) CADY ST FMS (SEE DETAIL)	(J) BRIDGE OUT R11-2B 48"x30"
DETOUR M4-8 24"x12"	END DETOUR M4-8A 24"x18"	
(K) BRIDGE OUT 0.2 MILES AHEAD LOCAL TRAFFIC ONLY R11-3B 60"x30"	(M) ROAD CLOSED AHEAD W20-3A	

LEGEND

-  PEDESTRIAN DETOUR ROUTE
-  WORK ZONE AREA
-  SIGN ON PERMANENT SUPPORT
-  SIGN ON TEMPORARY SUPPORT

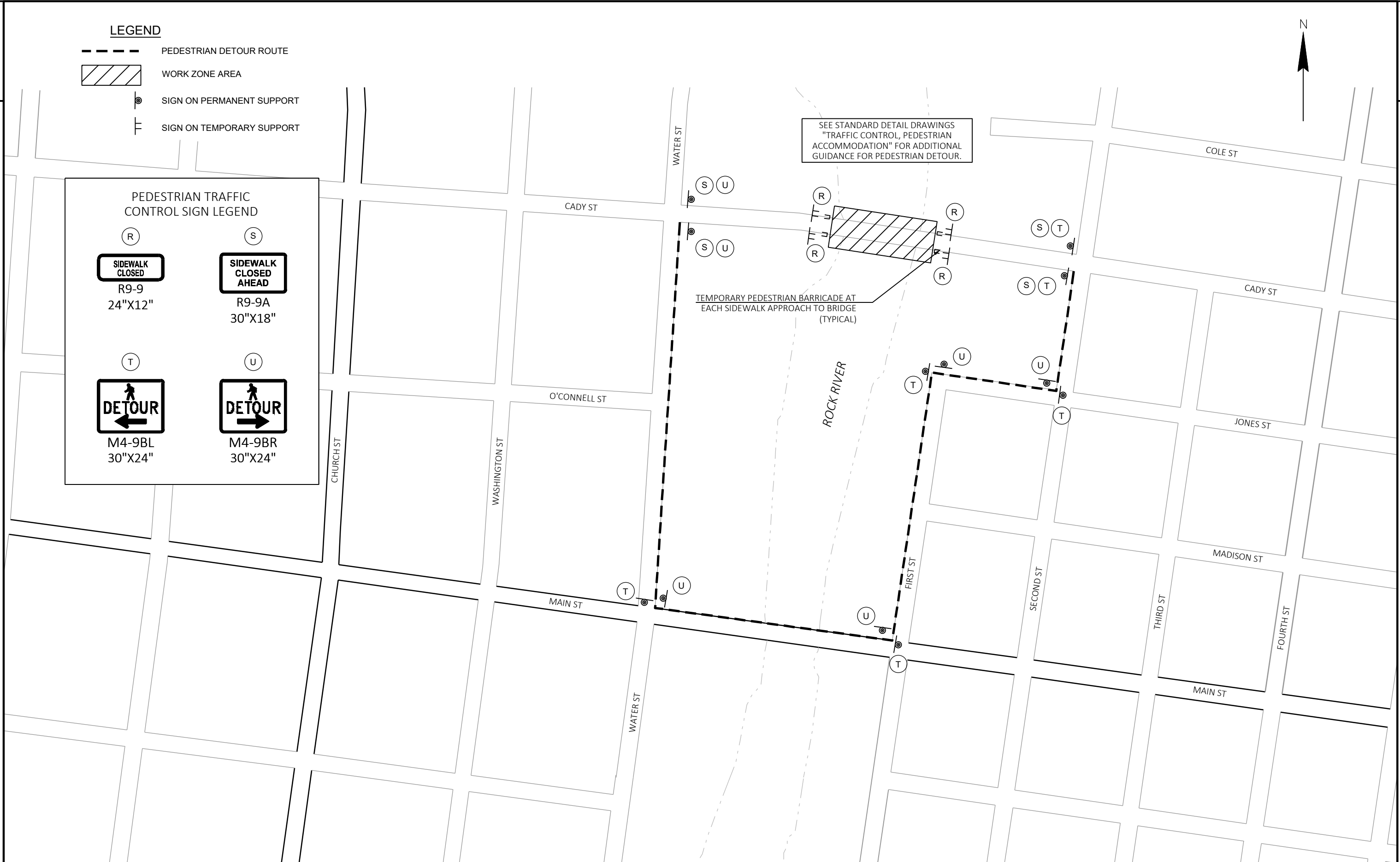


PEDESTRIAN TRAFFIC CONTROL SIGN LEGEND

<p>(R)</p> <div style="border: 1px solid black; padding: 2px; width: 60px; margin: 0 auto;">SIDEWALK CLOSED</div> <p>R9-9 24"X12"</p>	<p>(S)</p> <div style="border: 1px solid black; padding: 2px; width: 60px; margin: 0 auto;">SIDEWALK CLOSED AHEAD</div> <p>R9-9A 30"X18"</p>
<p>(T)</p> <div style="border: 1px solid black; padding: 2px; width: 60px; margin: 0 auto;">DETOUR</div> <p>M4-9BL 30"X24"</p>	<p>(U)</p> <div style="border: 1px solid black; padding: 2px; width: 60px; margin: 0 auto;">DETOUR</div> <p>M4-9BR 30"X24"</p>

SEE STANDARD DETAIL DRAWINGS
"TRAFFIC CONTROL, PEDESTRIAN
ACCOMMODATION" FOR ADDITIONAL
GUIDANCE FOR PEDESTRIAN DETOUR.

TEMPORARY PEDESTRIAN BARRICADE AT
EACH SIDEWALK APPROACH TO BRIDGE
(TYPICAL)



Estimate Of Quantities

3997-01-74

Line	Item	Item Description	Unit	Total	Qty
0002	204.0100	Removing Concrete Pavement	SY	42.000	42.000
0004	204.0115	Removing Asphaltic Surface Butt Joints	SY	12.000	12.000
0006	204.0120	Removing Asphaltic Surface Milling	SY	112.000	112.000
0008	204.0150	Removing Curb & Gutter	LF	36.000	36.000
0010	204.0155	Removing Concrete Sidewalk	SY	22.000	22.000
0012	205.0100	Excavation Common	CY	28.000	28.000
0014	213.0100	Finishing Roadway (project) 01. 3997-01-74	EACH	1.000	1.000
0016	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	52.000	52.000
0018	415.0120	Concrete Pavement 12-Inch	SY	8.000	8.000
0020	415.0410	Concrete Pavement Approach Slab	SY	94.000	94.000
0022	455.0605	Tack Coat	GAL	8.000	8.000
0024	465.0105	Asphaltic Surface	TON	20.000	20.000
0026	502.3200	Protective Surface Treatment	SY	506.000	506.000
0028	502.3215	Protective Surface Treatment Reseal	SY	205.000	205.000
0030	509.0301	Preparation Decks Type 1	SY	50.000	50.000
0032	509.0302	Preparation Decks Type 2	SY	25.000	25.000
0034	509.0505.S	Cleaning Decks to Reapply Concrete Masonry Overlay	SY	506.000	506.000
0036	509.1200	Curb Repair	LF	50.000	50.000
0038	509.1500	Concrete Surface Repair	SF	244.000	244.000
0040	509.2000	Full-Depth Deck Repair	SY	1.000	1.000
0042	509.2500	Concrete Masonry Overlay Decks	CY	42.000	42.000
0044	509.9005.S	Removing Concrete Masonry Deck Overlay (structure) 01. P-28-0706	SY	506.000	506.000
0046	601.0409	Concrete Curb & Gutter 30-Inch Type A	LF	36.000	36.000
0048	602.0415	Concrete Sidewalk 6-Inch	SF	180.000	180.000
0050	619.1000	Mobilization	EACH	1.000	1.000
0052	625.0100	Topsoil	SY	6.000	6.000
0054	627.0200	Mulching	SY	6.000	6.000
0056	628.1504	Silt Fence	LF	50.000	50.000
0058	628.1520	Silt Fence Maintenance	LF	100.000	100.000
0060	628.7015	Inlet Protection Type C	EACH	4.000	4.000
0062	629.0205	Fertilizer Type A	CWT	0.004	0.004
0064	630.0140	Seeding Mixture No. 40	LB	0.200	0.200
0066	643.0410	Traffic Control Barricades Type II	DAY	176.000	176.000
0068	643.0420	Traffic Control Barricades Type III	DAY	660.000	660.000
0070	643.0705	Traffic Control Warning Lights Type A	DAY	1,320.000	1,320.000
0072	643.0900	Traffic Control Signs	DAY	2,816.000	2,816.000
0074	643.1000	Traffic Control Signs Fixed Message	SF	100.000	100.000
0076	643.1050	Traffic Control Signs PCMS	DAY	14.000	14.000
0078	643.5000	Traffic Control	EACH	1.000	1.000
0080	644.1810	Temporary Pedestrian Barricade	LF	72.000	72.000
0082	646.1020	Marking Line Epoxy 4-Inch	LF	450.000	450.000
0084	650.4500	Construction Staking Subgrade	LF	30.000	30.000
0086	650.5000	Construction Staking Base	LF	30.000	30.000
0088	650.5500	Construction Staking Curb Gutter and Curb & Gutter	LF	16.000	16.000
0090	650.7000	Construction Staking Concrete Pavement	LF	30.000	30.000
0092	650.9500	Construction Staking Sidewalk (project) 01. 3997-01-74	EACH	1.000	1.000
0094	650.9911	Construction Staking Supplemental Control (project) 01. 3997-01-74	EACH	1.000	1.000
0096	690.0150	Sawing Asphalt	LF	57.000	57.000
0098	690.0250	Sawing Concrete	LF	34.000	34.000

Estimate Of Quantities

3997-01-74

Line	Item	Item Description	Unit	Total	Qty
0100	715.0502	Incentive Strength Concrete Structures	DOL	500.000	500.000
0102	715.0720	Incentive Compressive Strength Concrete Pavement	DOL	500.000	500.000
0104	999.2005.S	Maintaining Bird Deterrent System (station) 01. 20+00	EACH	1.000	1.000
0106	SPV.0060	Special 01. Replacement Manhole Cover	EACH	1.000	1.000

REMOVING CONCRETE PAVEMENT

				204.0100
CATEGORY	STA	LOCATION		SY
0010	19+18	CADY ST		42
TOTAL				42

REMOVING CONCRETE SIDEWALK

					204.0155
CATEGORY	STA	LOCATION	OFFSET		SY
0010	19+12	CADY ST	LT		4
0010	19+12	CADY ST	RT		5
0010	20+87	CADY ST	LT		6
0010	20+87	CADY ST	RT		7
TOTAL					22

CONCRETE PAVEMENT APPROACH SLAB

				415.0410
CATEGORY	STA	LOCATION		SY
0010	19+18	CADY ST		47
0010	20+83	CADY ST		47
TOTAL				94

REMOVING ASPHALTIC SURFACE BUTT JOINTS

				204.0115
CATEGORY	STA	LOCATION		SY
0010	18+88	CADY ST		6
0010	21+11	CADY ST		6
TOTAL				12

EXCAVATION COMMON

				205.0100
CATEGORY	STA	LOCATION		CY
0010	20+83	CADY ST		28
TOTAL				28

ASPHALTIC SURFACE

							455.0605	465.0105
							TACK	ASPHALTIC
							COAT	SURFACE
CATEGORY	STA	TO	STA	LOCATION			GAL	TON
0010	18+88	-	19+08	CADY ST			4	10
0010	20+91	-	21+11	CADY ST			4	10
TOTALS							8	20

REMOVING ASPHALTIC SURFACE MILLING

					204.0120
CATEGORY	STA	TO	STA	LOCATION	SY
0010	18+88	-	19+08	CADY ST	56
0010	20+91	-	21+11	CADY ST	56
TOTAL					112

BASE AGGREGATE DENSE 1 1/4-INCH

				305.0120
CATEGORY	STA	LOCATION		TON
0010	19+18	CADY ST		26
0010	20+83	CADY ST		26
TOTAL				52

CURB & GUTTER 30-INCH TYPE A

					601.0409
CATEGORY	STA	LOCATION	OFFSET		LF
0010	19+12	CADY ST	LT		8
0010	19+12	CADY ST	RT		9
0010	20+87	CADY ST	LT		9
0010	20+87	CADY ST	RT		10
TOTAL					36

REMOVING CURB & GUTTER

					204.0150
CATEGORY	STA	LOCATION	OFFSET		LF
0010	19+12	CADY ST	LT		8
0010	19+12	CADY ST	RT		9
0010	20+87	CADY ST	LT		9
0010	20+87	CADY ST	RT		10
TOTAL					36

CONCRETE PAVEMENT 12-INCH

					415.0120
CATEGORY	STA	LOCATION	OFFSET		SY
0010	19+20	CADY ST	LT		2
0010	19+20	CADY ST	RT		2
0010	20+80	CADY ST	LT		2
0010	20+80	CADY ST	RT		2
TOTAL					8

CONCRETE SIDEWALK 6-INCH

					602.0415
CATEGORY	STA	LOCATION	OFFSET		SF
0010	19+12	CADY ST	LT		31
0010	19+12	CADY ST	RT		45
0010	20+87	CADY ST	LT		47
0010	20+87	CADY ST	RT		57
TOTAL					180

3

LANDSCAPING

CATEGORY	STA	LOCATION	OFFSET	625.0100 627.0200 629.0205 630.0140			
				TOPSOIL SY	MULCHING SY	FERTILIZER TYPE A CWT	SEEDING MIXTURE NO. 40 LB
0010	19+12	CADY ST	LT	3	3	0.002	0.1
0010	20+87	CADY ST	RT	3	3	0.002	0.1
TOTAL				6	6	0.004	0.2

SILT FENCE

CATEGORY	LOCATION	628.1504 628.1520	
		SILT FENCE LF	SILT FENCE MAINTENANCE LF
0010	CADY ST NW	25	50
0010	CADY ST SE	25	50
TOTALS		50	100

INLET PROTECTION TYPE C

CATEGORY	LOCATION	OFFSET	628.7015
			EACH
0010	CADY ST	LT	1
0010	CADY ST	RT	1
0010	CADY ST	LT	1
0010	CADY ST	RT	1
TOTAL			4

MARKING LINE EPOXY 4-INCH

CATEGORY	STA	TO STA	LOCATION	646.1020	
				LF	REMARKS
0010	18+88	- 21+11	CADY ST	450	DOUBLE YELLOW
TOTAL				450	

STAKING

CATEGORY	STATION TO STATION	LOCATION	650.4500 650.5000 650.5500 650.7000 650.9500 650.9911					
			CONSTRUCTION STAKING SUBGRADE LF	CONSTRUCTION STAKING BASE LF	CONSTRUCTION STAKING CURB GUTTER AND LF	CONSTRUCTION STAKING CONCRETE PAVEMENT LF	CONSTRUCTION STAKING SIDEWALK 3997-01-74 EACH	CONSTRUCTION STAKING SUPPLEMENTAL CONTROL 3997-01-74 EACH
0010	18+88 - 21+11	MAINLINE	15	15	8	15	-	-
0010	19+24 - 20+76	MAINLINE	15	15	8	15	-	-
TOTALS			30	30	16	30	1	1

TRAFFIC CONTROL

CATEGORY	LOCATION	DAYS	* 643.0410		* 643.0420		* 643.0705		643.0900		643.1050		644.1810	
			NO.	DAY	NO.	DAY	NO.	DAY	NO.	DAY	NO.	DAY	NO.	DAY
0010	CADY ST ROAD CLOSURE	44	-	-	12	528	24	1,056	6	264	2	14	-	-
0010	CADY ST SIDEWALK CLOSURE	44	4	176	-	-	-	-	4	176	-	-	72	-
0010	PEDESTRIAN DETOUR ROUTE	44	-	-	-	-	-	-	16	704	-	-	-	-
TOTALS					176	528	1,056	1,144	14	72				

SAWING

CATEGORY	STA	LOCATION	OFFSET	690.0150 690.0250	
				LF	LF
0010	19+08	CADY ST	-	29	-
0010	19+08	CADY ST	LT	-	8
0010	19+08	CADY ST	RT	-	9
0010	20+91	CADY ST	LT	-	8
0010	20+91	CADY ST	RT	-	9
0010	20+91	CADY ST	-	28	-
TOTAL				57	34

* ADDITIONAL QUANTITIES SHOWN ELSEWHERE

3

TRAFFIC CONTROL DETOUR SIGN SUMMARY

SIGN NO.	LOCATION	SIGN CODE	SIZE W X H	NUMBER IN SERVICE	APPROX. SERVICE PERIOD 44 DAYS	* * *				REMARKS	
						643.0900 TRAFFIC CONTROL SIGNS	643.0420 TRAFFIC CONTROL BARRICADES TYPE III	643.0705 TRAFFIC CONTROL WARNING LIGHTS TYPE A	643.1000 TRAFFIC CONTROL SIGNS FIXED MESSAGE SF		
1	500 FT WEST OF CHURCH ST	W20-3A	48 X 48	1	44	44					
2	250 FT WEST OF CHURCH ST	FMS	30 X 15	1	44	-			3.125	SEE SIGN DETAIL	
	"	W20-2A	48 X 48	1	44	44					
3	50 FT WEST OF WATER ST	FMS	30 X 15	1	44	-			3.125	SEE SIGN DETAIL	
	"	M4-59R	30 X 30	1	44	44					
4	JUST EAST OF CHURCH ST	R11-3B	60 X 30	1	44	44	44	88		BRIDGE OUT 0.2 MILES	
5	250 FT WEST OF WATER ST	W20-3A	48 X 48	1	44	44					
6	JUST EAST OF WATER ST	R11-2B	48 X 30	1	44	44	44	88		BRIDGE OUT	
7	150 FT SOUTH OF CADY ST	FMS	30 X 15	1	44	-			3.125	SEE SIGN DETAIL	
	"	M4-8	24 X 12	1	44	44					
8	150 FT NORTH OF MAIN ST	FMS	30 X 15	1	44	-			3.125	SEE SIGN DETAIL	
	"	M4-9RA	30 X 24	1	44	44					
9	150 FT SOUTH OF MAIN ST	FMS	30 X 15	1	44	-			3.125	SEE SIGN DETAIL	
	"	M4-8	24 X 12	1	44	44					
10	250 FT NORTH OF MILWAUKEE ST	FMS	30 X 15	1	44	-			3.125	SEE SIGN DETAIL	
	"	M4-59L	30 X 30	1	44	44					
11	AT MILWAUKEE ST	FMS	30 X 15	1	44	-			3.125	SEE SIGN DETAIL	
	"	M4-9L	30 X 24	1	44	44					
12	150 FT EAST OF CHURCH ST	FMS	30 X 15	1	44	-			3.125	SEE SIGN DETAIL	
	"	M4-8	24 X 12	1	44	44					
13	250 FT WEST OF FIRST ST	FMS	30 X 15	1	44	-			3.125	SEE SIGN DETAIL	
	"	M4-59L	30 X 30	1	44	44					
14	AT FIRST ST	FMS	30 X 15	1	44	-			3.125	SEE SIGN DETAIL	
	"	M4-9L	30 X 24	1	44	44					
15	150 FT NORTH OF MILWAUKEE ST	FMS	30 X 15	1	44	-			3.125	SEE SIGN DETAIL	
	"	M4-8	24 X 12	1	44	44					
16	150 FT SOUTH OF MAIN ST	FMS	30 X 15	1	44	-			3.125	SEE SIGN DETAIL	
	"	M4-9RA	30 X 24	1	44	44					
17	AT MADISON ST	FMS	30 X 15	1	44	-			3.125	SEE SIGN DETAIL	
	"	M4-9R	30 X 24	1	44	44					
18	AT SECOND ST	FMS	30 X 15	1	44	-			3.125	SEE SIGN DETAIL	
	"	M4-9L	30 X 24	1	44	44					
19	150 FT SOUTH OF CADY ST	FMS	30 X 15	1	44	-			3.125	SEE SIGN DETAIL	
	"	M4-8A	24 X 18	1	44	44					
21	500 FT EAST OF SECOND ST	W20-3A	48 X 48	1	44	44					
22	250 FT EAST OF SECOND ST	FMS	30 X 15	1	44	-			3.125	SEE SIGN DETAIL	
	"	W20-2A	48 X 48	1	44	44					
23	100 FT EAST OF SECOND ST	FMS	30 X 15	1	44	-			3.125	SEE SIGN DETAIL	
	"	M4-59L	30 X 30	1	44	44					
24	AT SECOND ST	FMS	30 X 15	1	44	-			3.125	SEE SIGN DETAIL	
	"	M4-9L	30 X 24	1	44	44					
25	JUST WEST OF SECOND ST	R11-2B	48 X 30	1	44	44	44	88		BRIDGE OUT	
PAGE SUBTOTALS						1,056	132	264	56.25		

* ADDITIONAL QUANTITIES SHOWN ELSEWHERE

PROJECT NO: 3997-01-04

HWY: CADY STREET

COUNTY: JEFFERSON

MISCELLANEOUS QUANTITIES

SHEET:

E

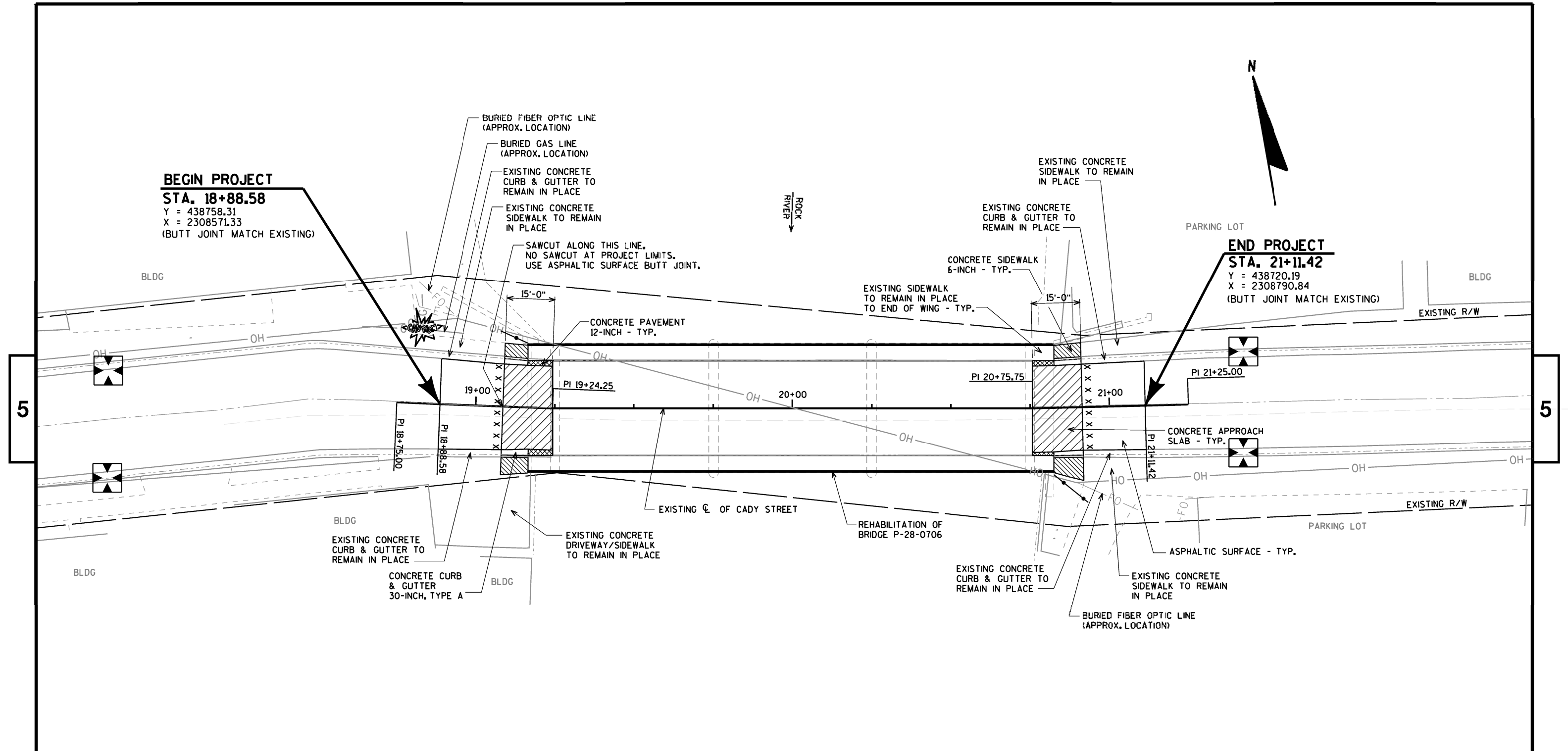
TRAFFIC CONTROL DETOUR SIGN SUMMARY

SIGN NO.	LOCATION	SIGN CODE	SIZE W X H	NUMBER IN SERVICE	APPROX. SERVICE PERIOD 44 DAYS	643.0900	643.0420	643.0705	643.1000	REMARKS
						TRAFFIC CONTROL SIGNS	TRAFFIC CONTROL BARRICADES TYPE III	TRAFFIC CONTROL WARNING LIGHTS TYPE A	TRAFFIC CONTROL SIGNS FIXED MESSAGE SF	
26	150 FT SOUTH OF CADY ST	FMS	30 X 15	1	44	-			3.125	SEE SIGN DETAIL
	"	M4-8	24 X 12	1	44	44				
27	50 FT NORTH OF MADISON ST	FMS	30 X 15	1	44	-			3.125	SEE SIGN DETAIL
	"	M4-9R	30 X 24	1	44	44				
28	50 FT EAST OF FIRST ST	FMS	30 X 15	1	44	-			3.125	SEE SIGN DETAIL
	"	M4-9L	30 X 24	1	44	44				
29	150 FT NORTH OF MAIN ST	FMS	30 X 15	1	44	-			3.125	SEE SIGN DETAIL
	"	M4-9RA	30 X 24	1	44	44				
30	150 FT SOUTH OF MAIN ST	FMS	30 X 15	1	44	-			3.125	SEE SIGN DETAIL
	"	M4-8	24 X 12	1	44	44				
31	250 FT NORTH OF MILWAUKEE ST	FMS	30 X 15	1	44	-			3.125	SEE SIGN DETAIL
	"	M4-59R	30 X 30	1	44	44				
32	AT MILWAUKEE ST	FMS	30 X 15	1	44	-			3.125	SEE SIGN DETAIL
	"	M4-9R	30 X 24	1	44	44				
33	150 FT WEST OF FIRST ST	FMS	30 X 15	1	44	-			3.125	SEE SIGN DETAIL
	"	M4-8	24 X 12	1	44	44				
34	250 FT EAST OF CHURCH ST	FMS	30 X 15	1	44	-			3.125	SEE SIGN DETAIL
	"	M4-59R	30 X 30	1	44	44				
35	AT CHURCH ST	FMS	30 X 15	1	44	-			3.125	SEE SIGN DETAIL
	"	M4-9R	30 X 24	1	44	44				
36	150 FT NORTH OF MILWAUKEE ST	FMS	30 X 15	1	44	-			3.125	SEE SIGN DETAIL
	"	M4-8	24 X 12	1	44	44				
37	150 FT SOUTH OF MAIN ST	FMS	30 X 15	1	44	-			3.125	SEE SIGN DETAIL
	"	M4-9RA	30 X 24	1	44	44				
38	150 FT NORTH OF MAIN ST	FMS	30 X 15	1	44	-			3.125	SEE SIGN DETAIL
	"	M4-8	24 X 12	1	44	44				
39	250 FT SOUTH OF CADY ST	FMS	30 X 15	1	44	-			3.125	SEE SIGN DETAIL
	"	M4-8A	24 X 18	1	44	44				
PAGE SUBTOTALS						616	0	0	43.75	
0010 PROJECT TOTALS						1,672	132	264	100.00	

* ADDITIONAL QUANTITIES SHOWN ELSEWHERE

BEGIN PROJECT
STA. 18+88.58
 Y = 438758.31
 X = 2308571.33
 (BUTT JOINT MATCH EXISTING)

END PROJECT
STA. 21+11.42
 Y = 438720.19
 X = 2308790.84
 (BUTT JOINT MATCH EXISTING)

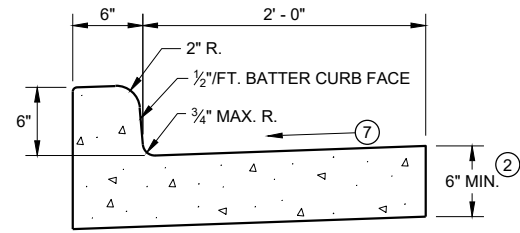


LEGEND

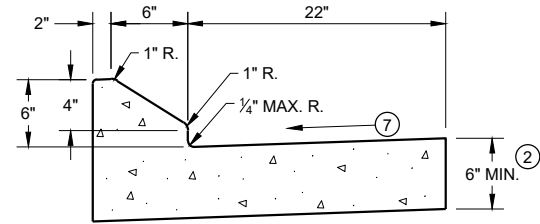
- SILT FENCE
- ⊗ INLET PROTECTION

Standard Detail Drawing List

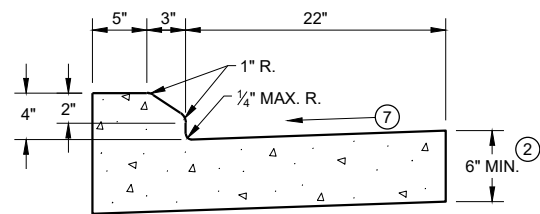
08D01-22A	CONCRETE CURB & GUTTER
08E09-06	SILT FENCE
08E10-02	INLET PROTECTION TYPE A, B, C AND D
13B02-09A	CONCRETE PAVEMENT APPROACH SLAB
13C01-19	CONCRETE PAVEMENT LONGITUDINAL JOINTS AND TIES
15C02-08A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-08B	BARRICADES AND SIGNS FOR VARIOUS CLOSURES
15C08-22A	LONGITUDINAL MARKING (MAINLINE)
15D30-08A	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-08B	TRAFFIC CONTROL, TEMPORARY ADA COMPLIANT PEDESTRIAN ACCOMMODATION
15D30-08C	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-08F	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION



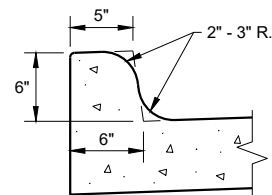
TYPES A^① & D



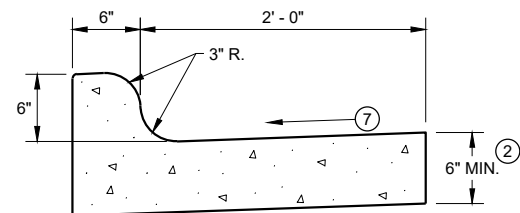
6" SLOPED CURB TYPES G^① & J



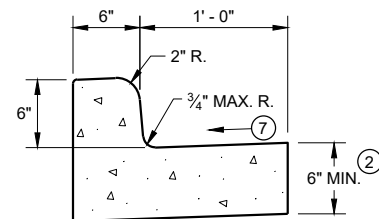
4" SLOPED CURB TYPES G^① & J



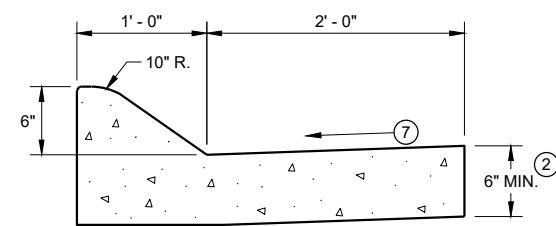
TYPES K^① & L
(OPTIONAL CURB SHAPE)



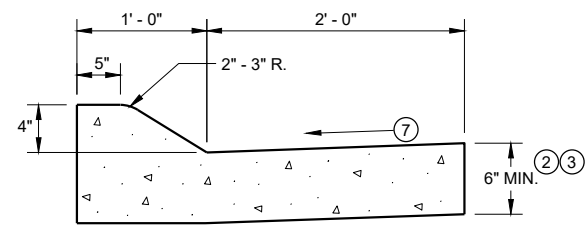
TYPES K^① & L
CONCRETE CURB AND GUTTER 30"



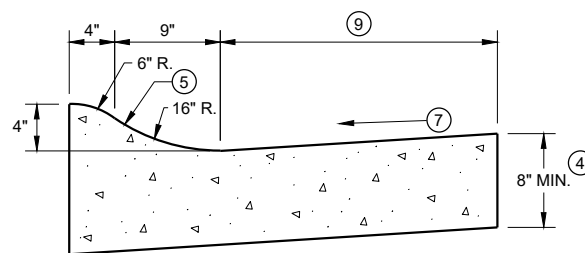
TYPES A^① & D
CONCRETE CURB AND GUTTER 18"



6" SLOPED CURB TYPES A^① & D

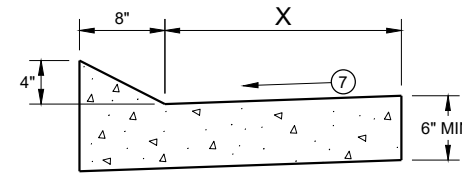


4" SLOPED CURB TYPES A^① & D
CONCRETE CURB AND GUTTER 36"



4" SLOPED CURB TYPES R^① & T

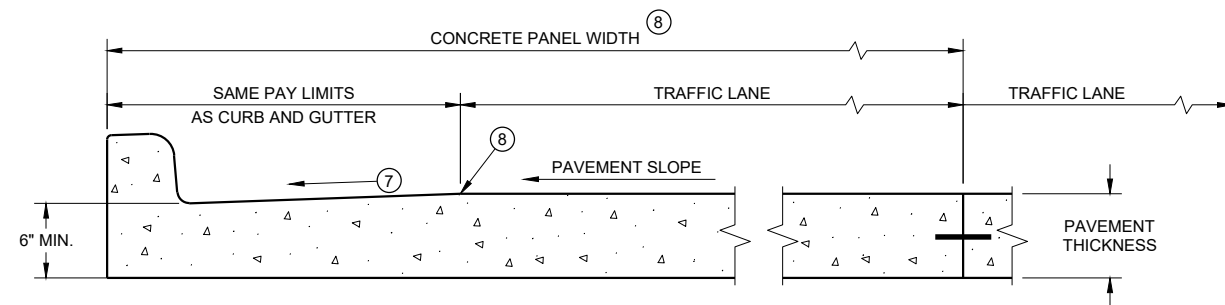
TBT & TBTT	X
30"	22"
36"	28"



TYPES TBT & TBTT^①
CONCRETE CURB AND GUTTER

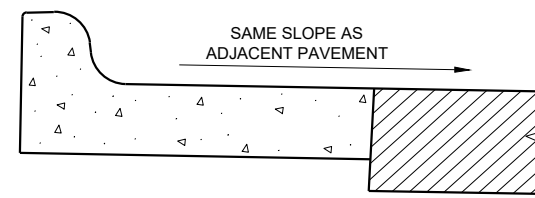
PAVEMENT THICKNESS
AND MAXIMUM CONCRETE
PANEL WIDTH TABLE

PAVEMENT THICKNESS	MAXIMUM PANEL WIDTH
LESS THAN 10"	12'
10" & ABOVE	15'



PARTIAL SECTION OF PAVEMENT *
WITH INTEGRAL CURB AND GUTTER

* BIKE LANE IS NOT SHOWN



REVERSE SLOPE GUTTER^⑥
(TYPICAL FOR ALL CURB & GUTTER TYPES)

GENERAL NOTES

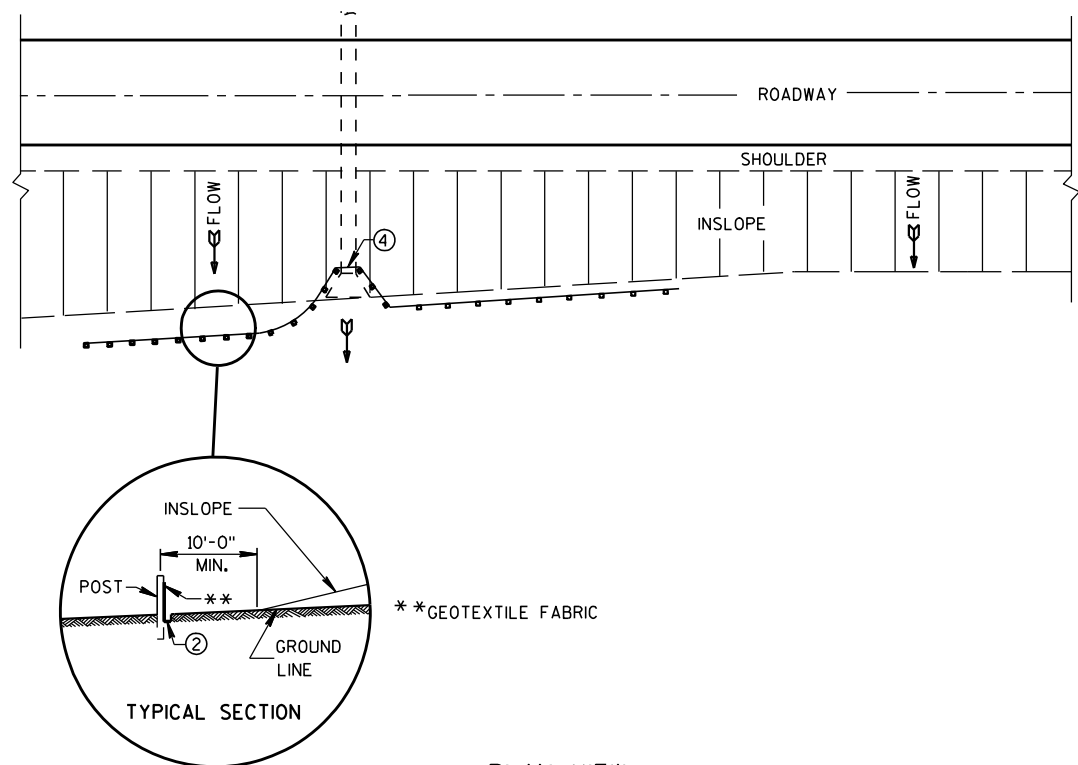
DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

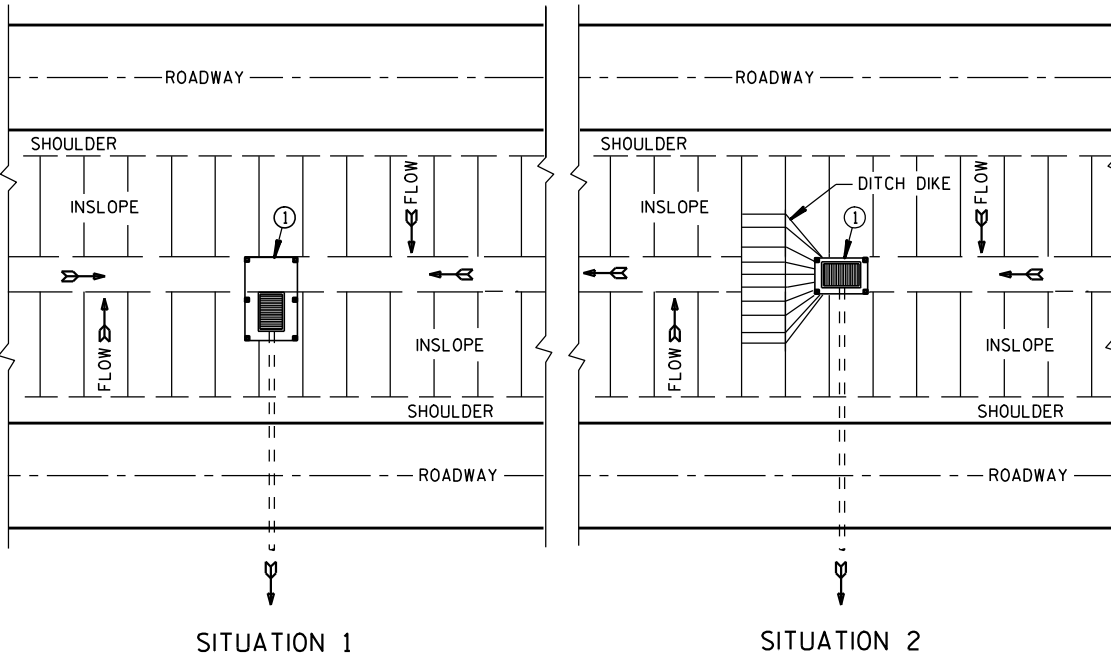
INTEGRAL CURB AND GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB AND GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ③ USE 8" MINIMUM GUTTER THICKNESS WHEN USED WITH AN ADJACENT CONCRETE TRUCK APRON PLACED BEHIND BACK OF CURB.
- ④ THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑤ UNLESS OTHERWISE NOTED, FOR STAKING PURPOSES THE FACE OF CURB IS 6" FROM THE BACK OF CURB.
- ⑥ WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.
- ⑦ USE 4% GUTTER CROSS SLOPE UNLESS OTHERWISE NOTED IN THE PLANS.
- ⑧ INCLUDE LONGITUDINAL JOINT AND TIE BARS ALONG LANE EDGE WHEN CONCRETE PANEL WIDTH EXCEEDS THE MAXIMUM WIDTH PER TABLE BELOW. LONGITUDINAL JOINT(S) ARE NOT ALLOWED WITHIN TRAFFIC LANES AND BIKE LANES. LONGITUDINAL JOINT MAY BE SAWED.
- ⑨ CONCRETE CURB AND GUTTER 4-INCH SLOPED 30-INCH TYPE "R" AND "T" = 17 INCHES
CONCRETE CURB AND GUTTER 4-INCH SLOPED 36-INCH TYPE "R" AND "T" = 23 INCHES



PLAN VIEW
TYPICAL APPLICATION OF SILT FENCE

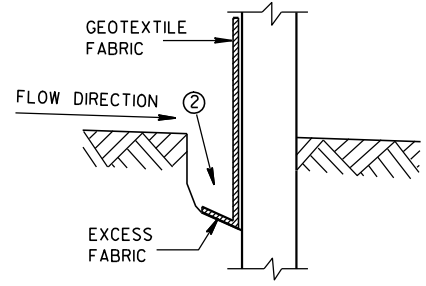


SITUATION 1 SITUATION 2
PLAN VIEW
SILT FENCE AT MEDIAN SURFACE DRAINS

GENERAL NOTES

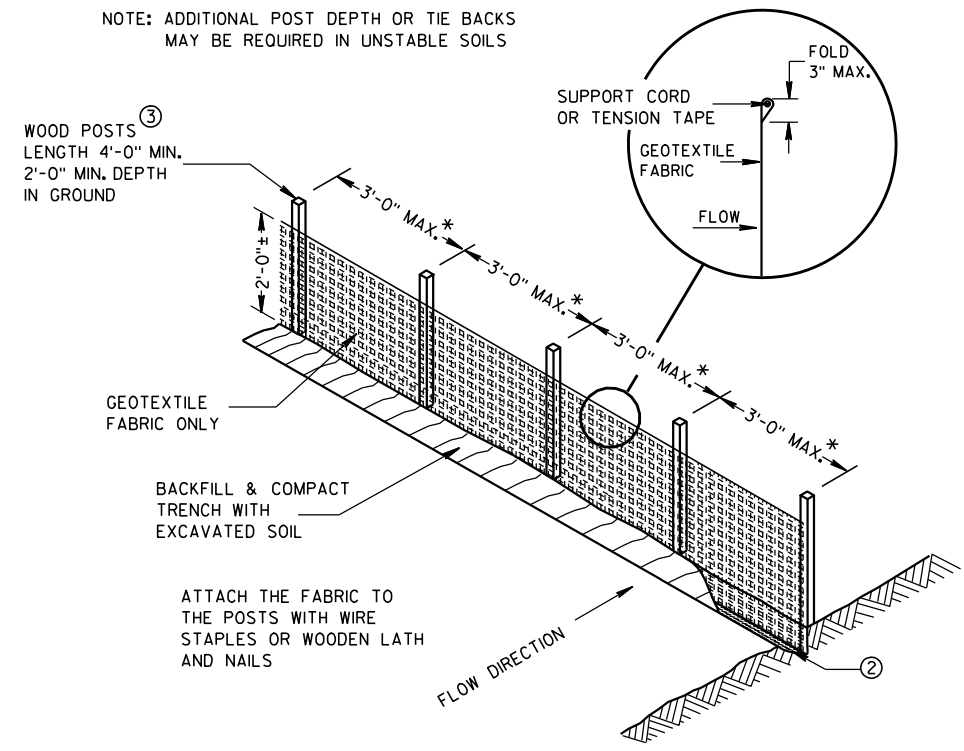
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

- ① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- ③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1 1/8" X 1 1/8" OF OAK OR HICKORY.
- ④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- ⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



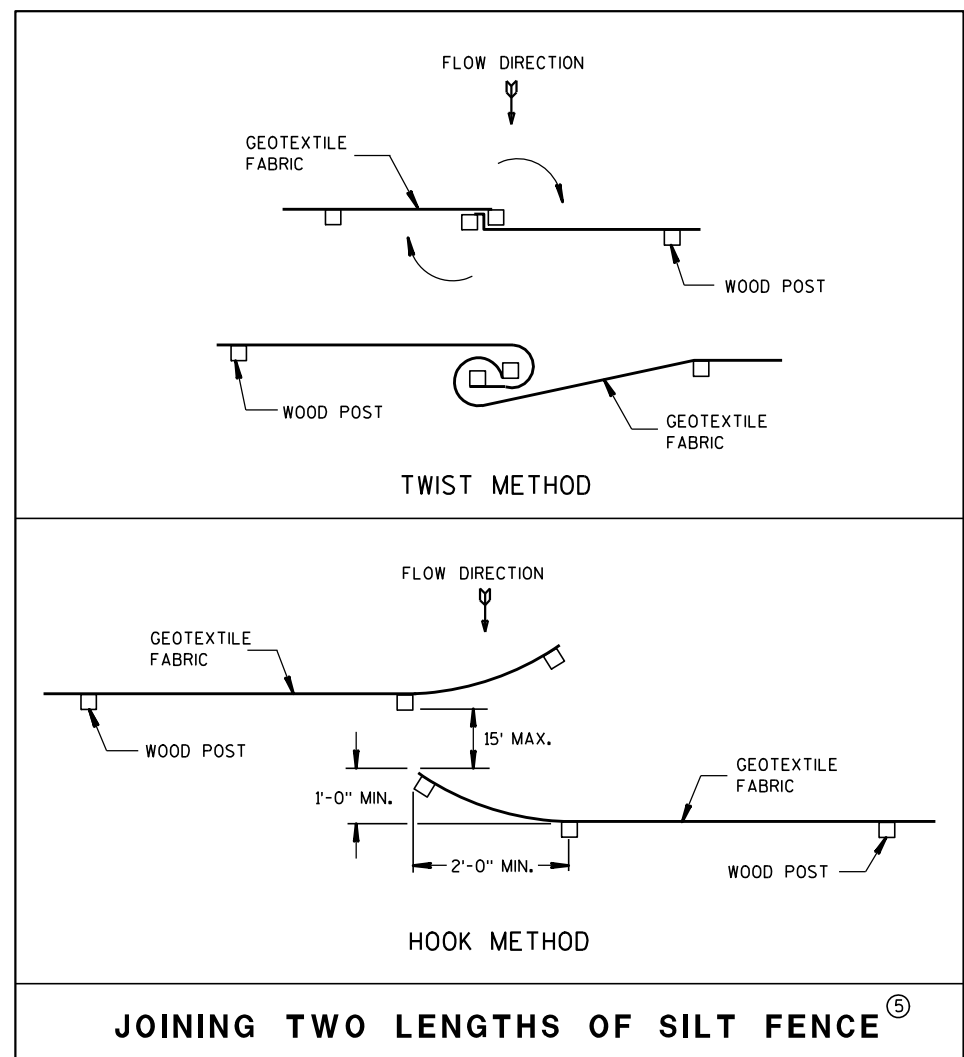
TRENCH DETAIL

NOTE: ADDITIONAL POST DEPTH OR TIE BACKS MAY BE REQUIRED IN UNSTABLE SOILS

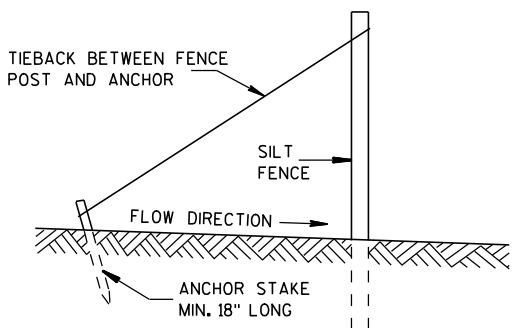


SILT FENCE

* NOTE: 8'-0" POST SPACING ALLOWED IF A WOVEN GEOTEXTILE FABRIC IS USED.

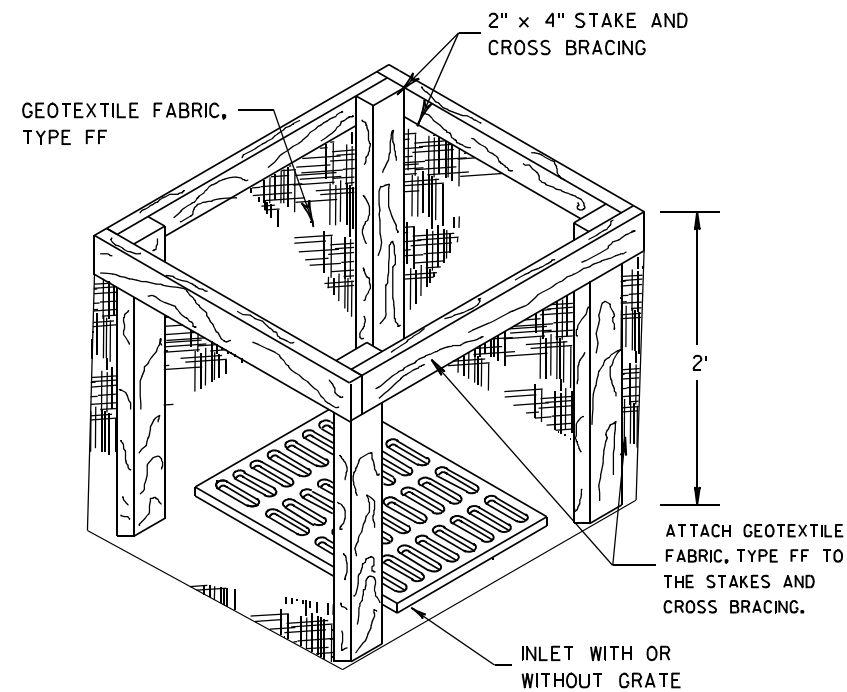
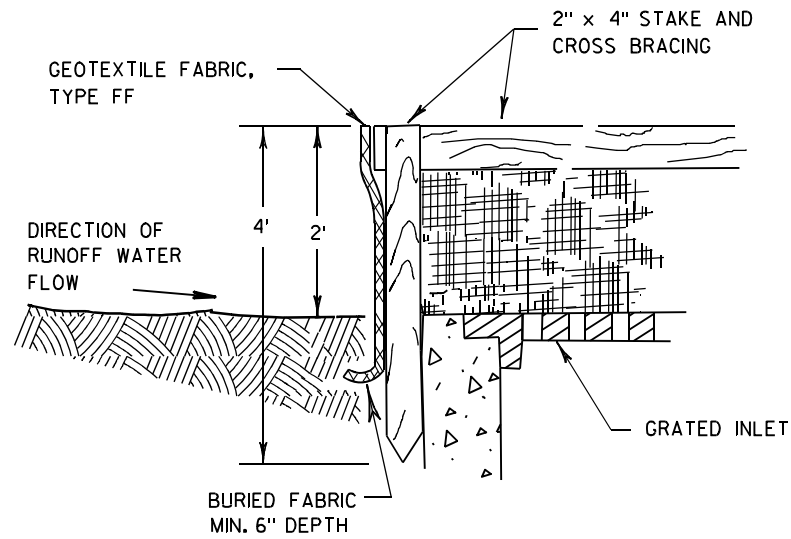


JOINING TWO LENGTHS OF SILT FENCE ⑤



SILT FENCE TIE BACK
(WHEN REQUIRED BY THE ENGINEER)

SILT FENCE	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 4-29-05 DATE	/S/ Beth Canestra CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA	



INLET PROTECTION, TYPE A

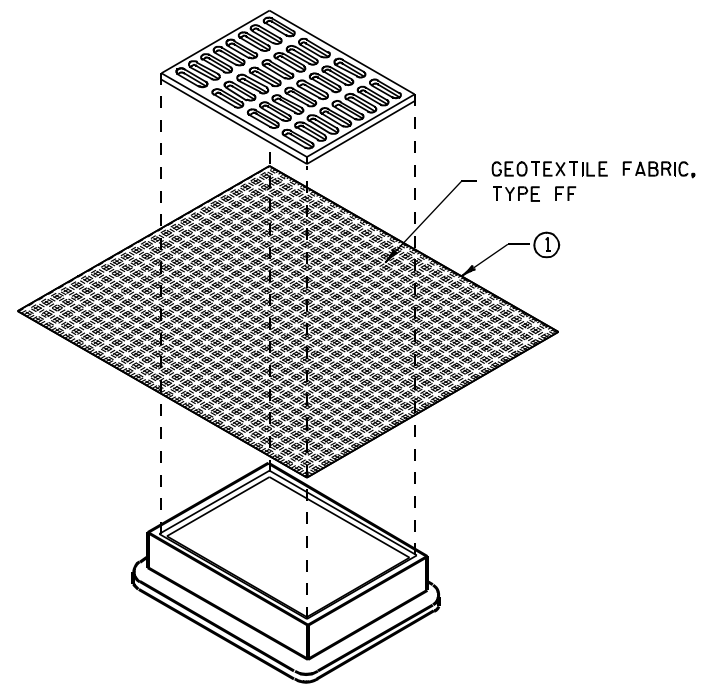
GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

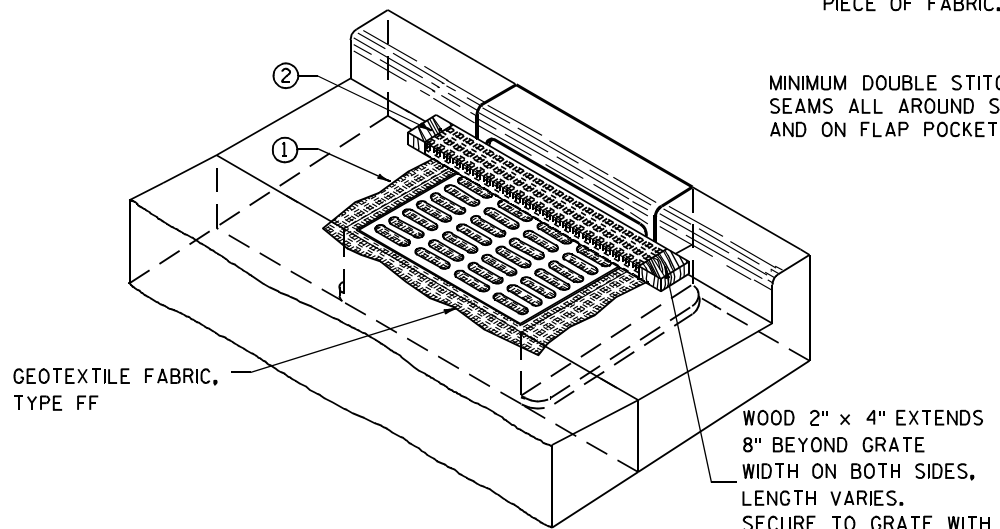
WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- ① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- ② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- ③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



**INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)**

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE C (WITH CURB BOX)

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

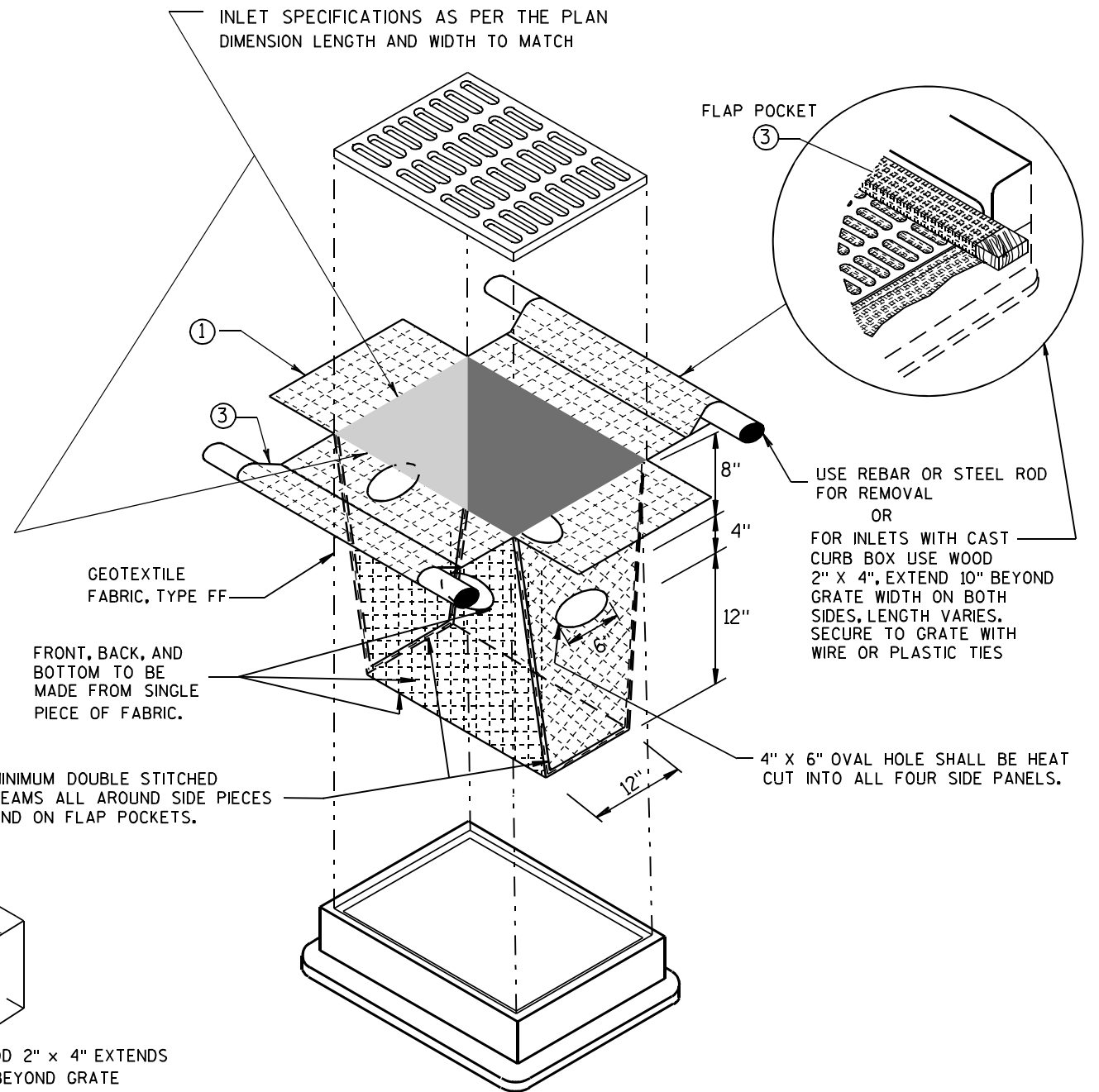
THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

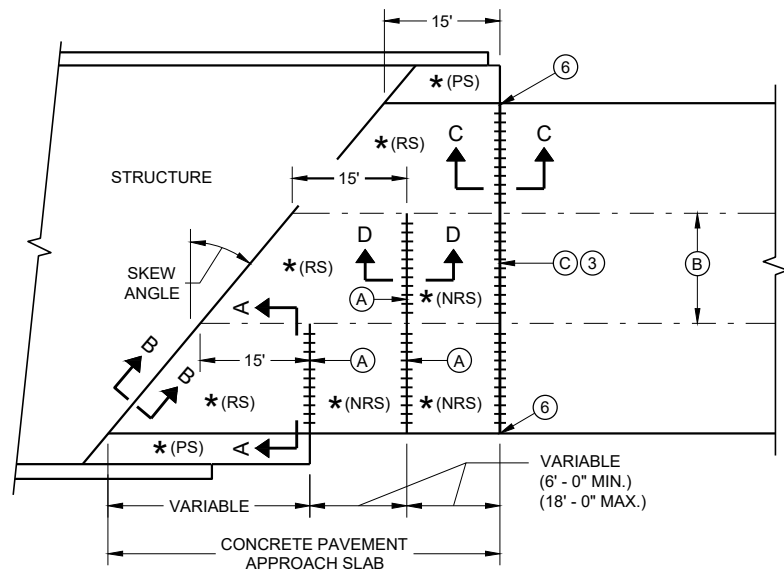
THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.



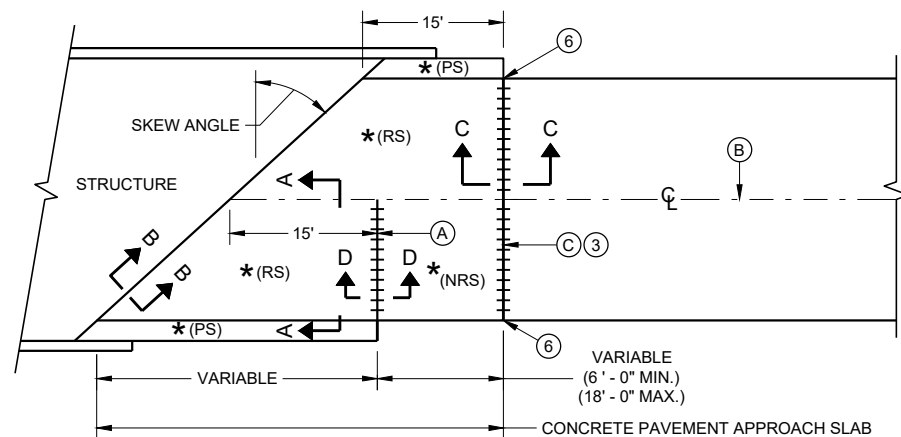
INLET PROTECTION, TYPE D

(CAN BE INSTALLED IN ANY INLET TYPE WITH OR WITHOUT A CURB BOX AS PER NOTE ②)

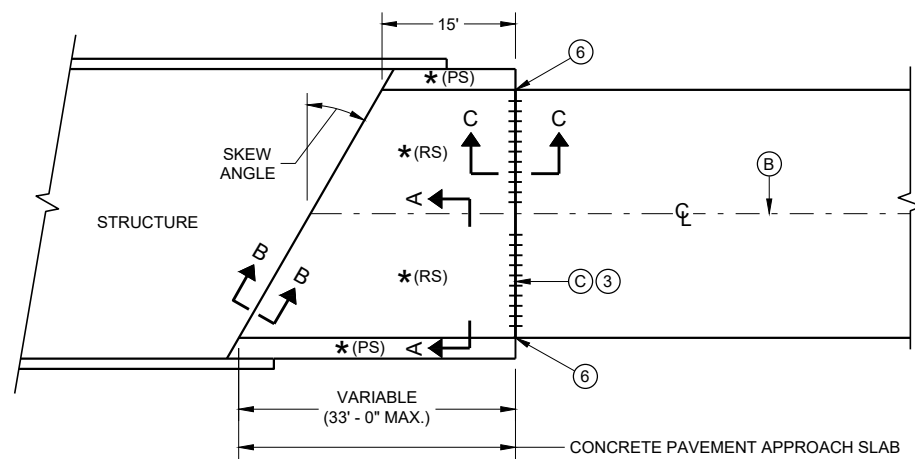
INLET PROTECTION TYPE A, B, C, AND D	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 10/16/02 DATE	/S/ Beth Conestra CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA	



**SKewed Approach
(Pavement More Than Two Lanes)**

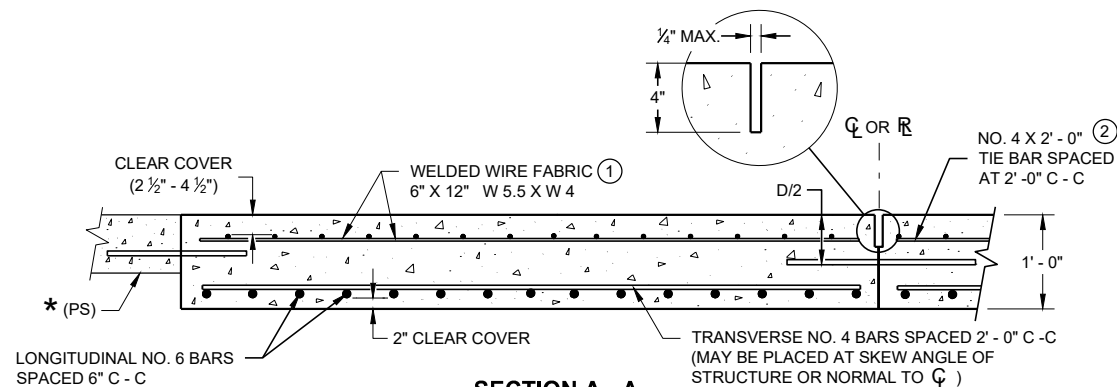


**SKews > 20°
(Pavement Width ≤ 30')**

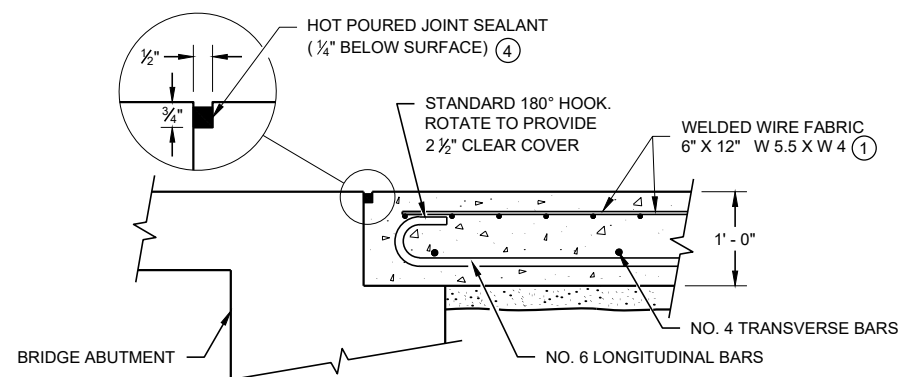


**SKews ≤ 20°
(Pavement Width ≤ 30')**
Approach Slab and Adjacent Pavement

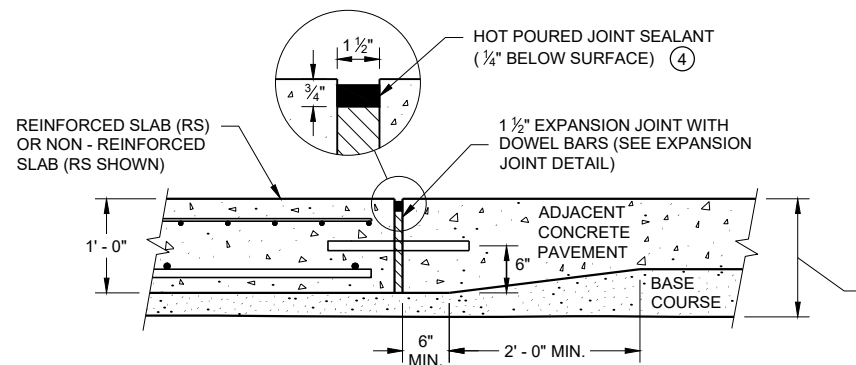
- * (RS) = REINFORCED CONCRETE SLAB
- * (PS) = PAVED CONCRETE SHOULDER OR CONCRETE DRAINAGE SLAB
- * (NRS) = NON - REINFORCED CONCRETE SLAB
- *** STANDARD DOWEL BAR DIAMETER (SEE SDD 13C11 AND SDD 13C13)



**SECTION A - A
REINFORCEMENT POSITIONING DETAIL**



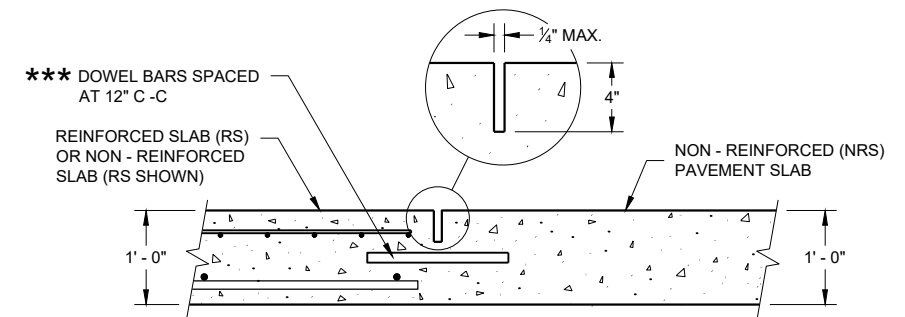
**SECTION B - B
BEND DETAIL
BOTTOM REINFORCEMENT**



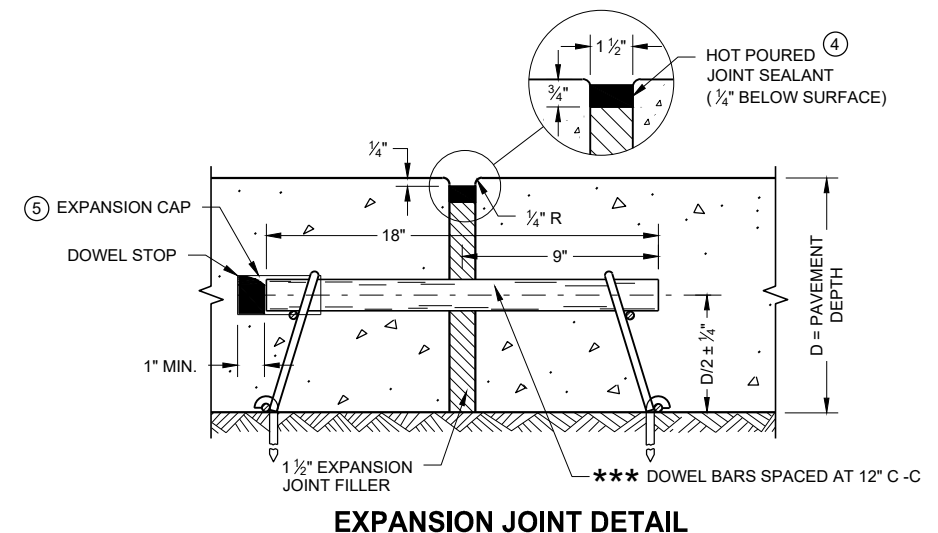
**SECTION C - C
TRANSITION DETAIL
Approach Slab to Adjacent Pavement**

GENERAL NOTES

- THE CONTRACTOR MAY SPLICE NO. 6 BARS IN THE APPROACH SLAB FOR SKEWED STRUCTURES ONLY. STAGGER SPLICES WITH A MAXIMUM OF ONE SPLICE PER BAR. THE LENGTH OF LAP IS 20 INCHES.
- TACK WELD DOWEL BARS TO THE BASKETS ON ALTERNATE ENDS.
- ① THE CONTRACTOR MAY USE NO. 4 BARS SPACED AT 2' - 0" C - C IN BOTH THE LONGITUDINAL AND TRANSVERSE DIRECTIONS FOR TOP REINFORCEMENT AS AN ALTERNATIVE TO THE WELDED WIRE FABRIC.
 - ② THE CONTRACTOR MAY OMIT THE BARS BETWEEN REINFORCED SLABS WHERE SLAB REINFORCEMENT BARS EXTEND ACROSS THE CENTERLINE OR REFERENCE LINE.
 - ③ DO NOT CONSTRUCT AN EXPANSION JOINT OR INSTALL DOWEL BARS WHEN ABUTTING AN HMA PAVEMENT.
 - ④ USE A JOINT SEALANT CONFORMING TO STANDARD SPECIFICATION 415.2.6.
 - ⑤ PLACE EXPANSION CAP ON THE END OF THE DOWEL THAT IS NOT TACK WELDED TO THE BASKET. DO NOT FORCE DOWEL BAR PAST THE DOWEL STOP.
 - ⑥ EXTEND EXPANSION JOINT THROUGH ANY ADJACENT TIED CONCRETE.
 - (A) STANDARD CONTRACTION JOINT NORMAL TO \bar{C} OR \bar{R} .
 - (B) STANDARD LONGITUDINAL JOINT WITH TIE BARS.
 - (C) 1 1/2" EXPANSION JOINT WITH DOWEL BARS NORMAL TO \bar{C} OR \bar{R} .



**SECTION D - D
CONTRACTION JOINT**



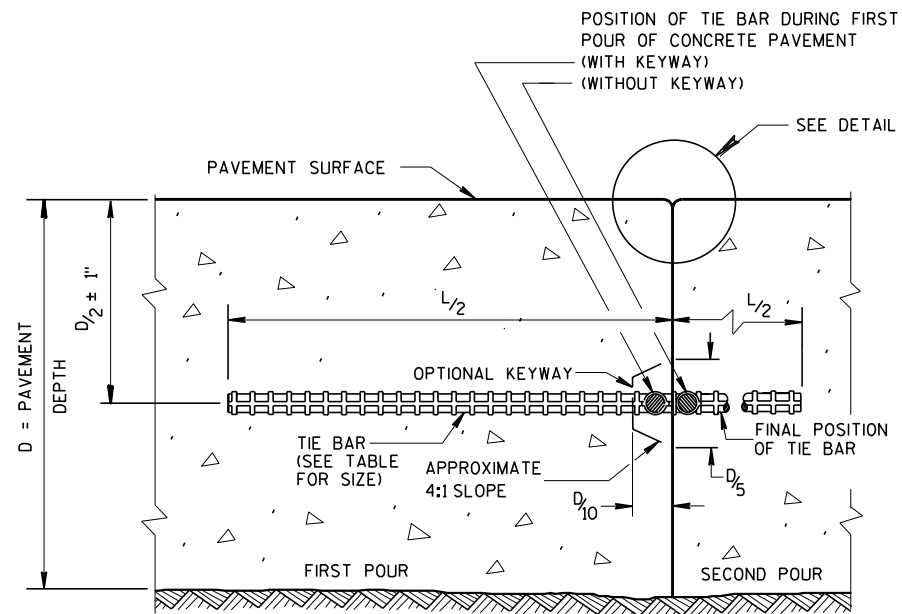
EXPANSION JOINT DETAIL

**CONCRETE PAVEMENT
APPROACH SLAB**

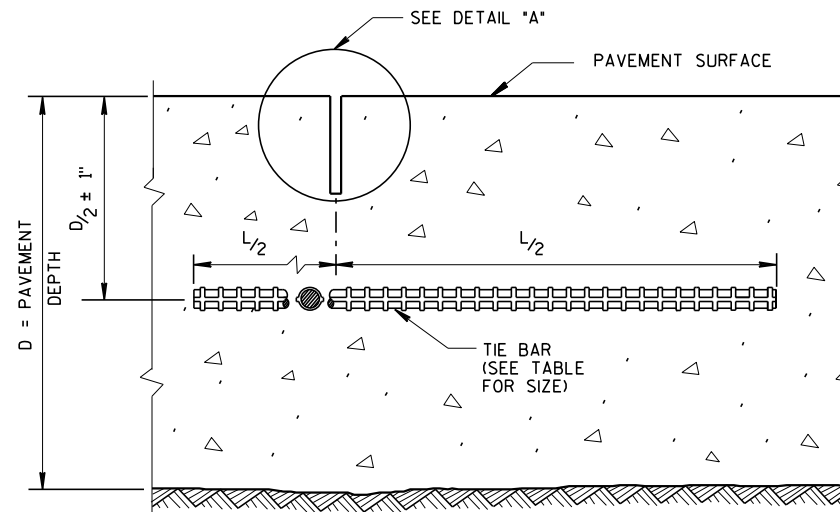
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2018 /S/ Peter Kemp, P.E.
DATE DATE PAVEMENT SUPERVISOR

FHWA



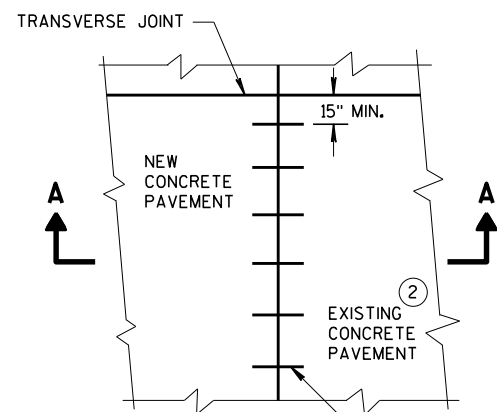
CONSTRUCTION JOINT



SAWED JOINT

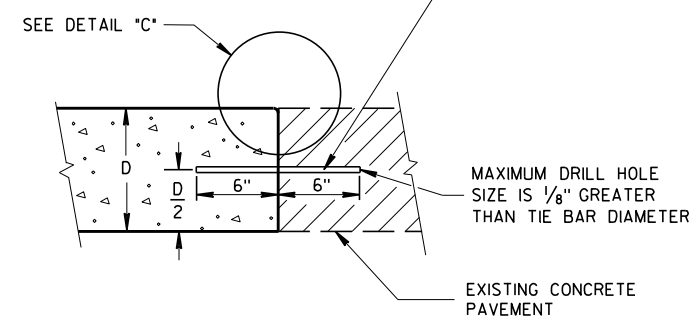
GENERAL NOTES

- CREATE A LONGITUDINAL JOINT FOR PAVEMENT WIDTHS GREATER THAN 15 FEET.
- CORRELATE LONGITUDINAL JOINTS WITH LANE LINES WHEN POSSIBLE.
- ① ANCHOR TIE BARS INTO DRILLED HOLES WITH AN EPOXY.
- ② PAVEMENT THAT WAS IN PLACE PRIOR TO THE CONTRACT.

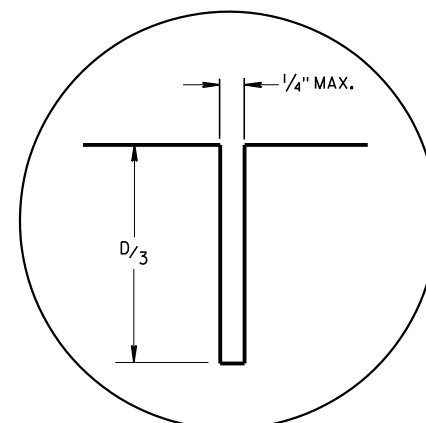


PLAN VIEW

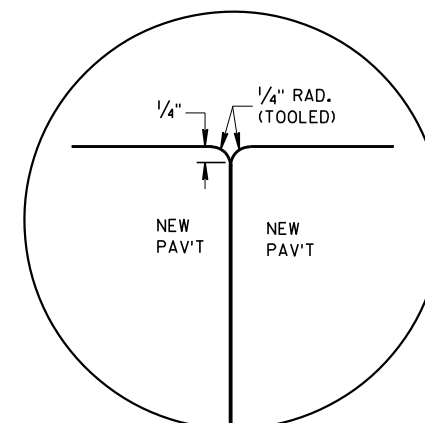
NO. 6 TIE BARS SPACED 30" C-C, INSTALLED PERPENDICULAR TO THE LONGITUDINAL JOINT. ①



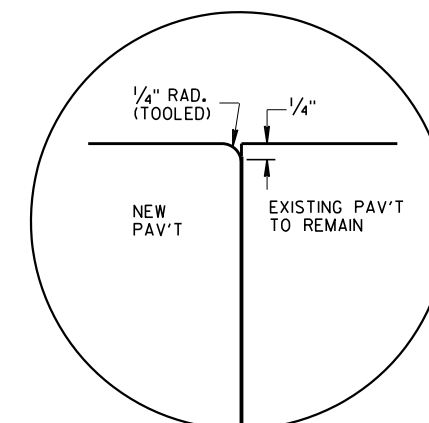
**SECTION A-A
LONGITUDINAL CONSTRUCTION JOINT
TIE BARS ANCHORED
INTO EXISTING PAVEMENT**



DETAIL "A"



DETAIL "B"



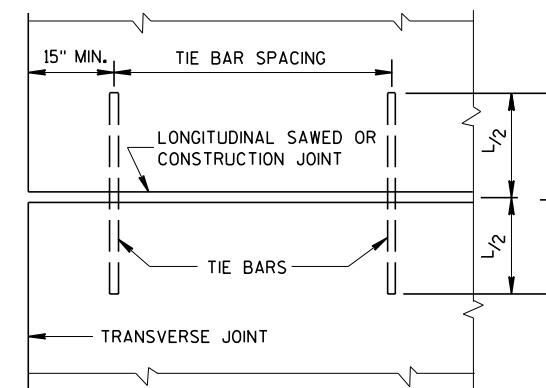
DETAIL "C"

TIE BAR TABLE

PAVEMENT DEPTH (D)	TIE BAR SIZE	TIE BAR LENGTH (L)	MAX. TIE BAR SPACING
< 10 1/2"	NO. 4	30"	36"
≥ 10 1/2"	NO. 5	36"	36"
	NO. 4 *	30"	24" **

* SUBSTITUTE BENT BARS AT LONGITUDINAL JOINTS WHEN EQUIPMENT LIMITATIONS DURING CONSTRUCTION WARRANT (e.g. AUXILIARY LANES OR TURN LANES)

** CONFORM TO 15" MINIMUM SPACING FROM TRANSVERSE JOINTS; SPACING BETWEEN TIE BARS WILL BE 30" AT TRANSVERSE JOINTS.

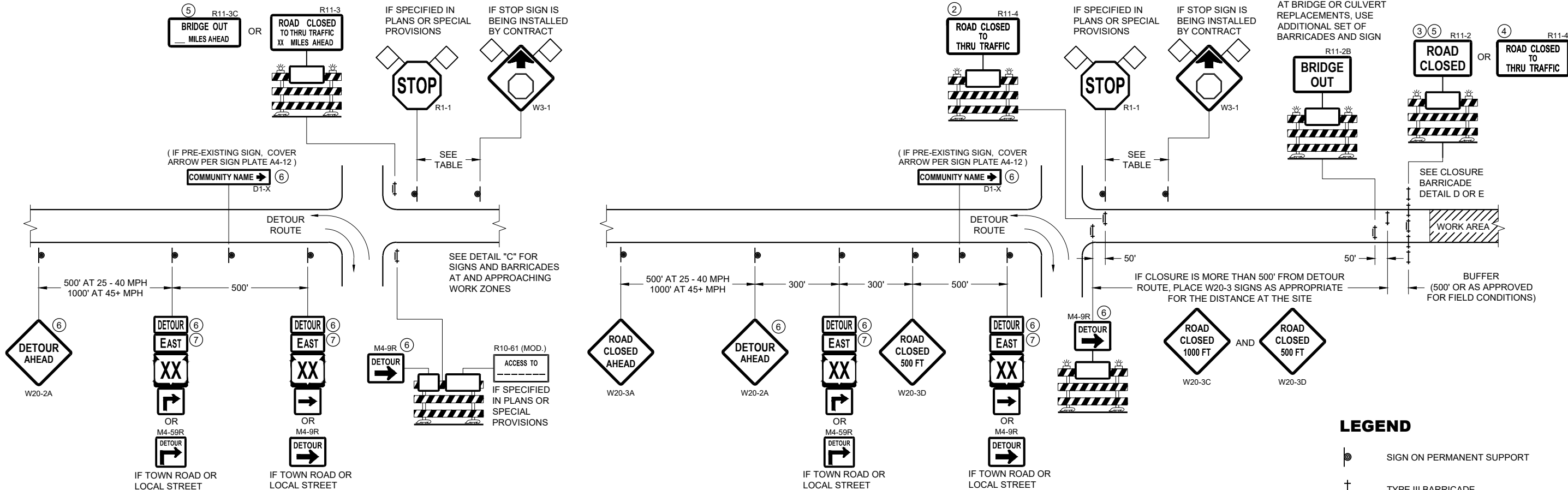


**PLAN VIEW
SHOWING LOCATION OF TIE BARS**

**CONCRETE PAVEMENT
LONGITUDINAL JOINTS AND TIES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
March 2018 /S/ Peter Kemp, P.E.
DATE PAVEMENT SUPERVISOR
FHWA



DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR

WORK ZONE GREATER THAN OR EQUAL TO 1/2 MILE FROM
 DETOUR ROUTE (1000 FEET IF URBAN)

DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR

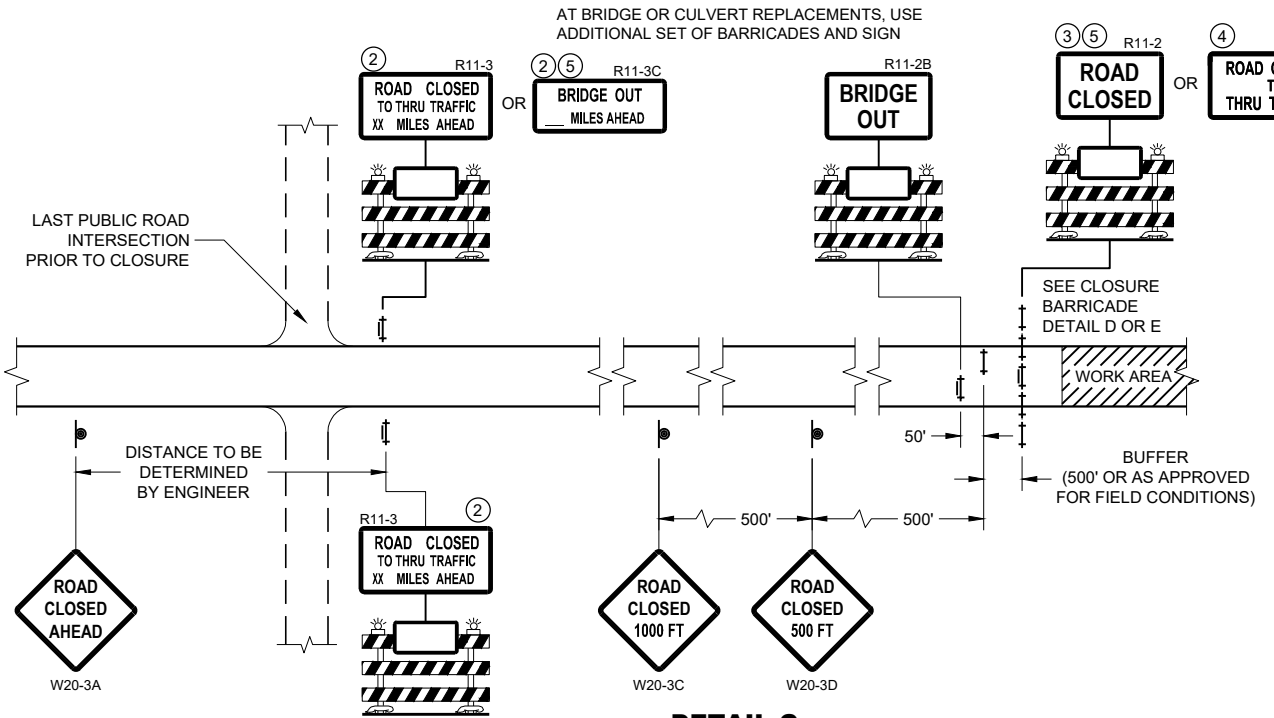
WORK ZONE LESS THAN 1/2 MILE FROM
 DETOUR ROUTE (1000 FEET IF URBAN)

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
- FLAGS, 16" X 16" MIN. (ORANGE)
- M4 - 8
- M3 - X
- M1 - 4 OR M1 - 6 OR M1 - 5A
- M05 - 1 OR M06 - 1

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

SEE SDD 15C2-SHEET "b"
 FOR GENERAL NOTES
 AND FOOTNOTES ① THROUGH ⑦

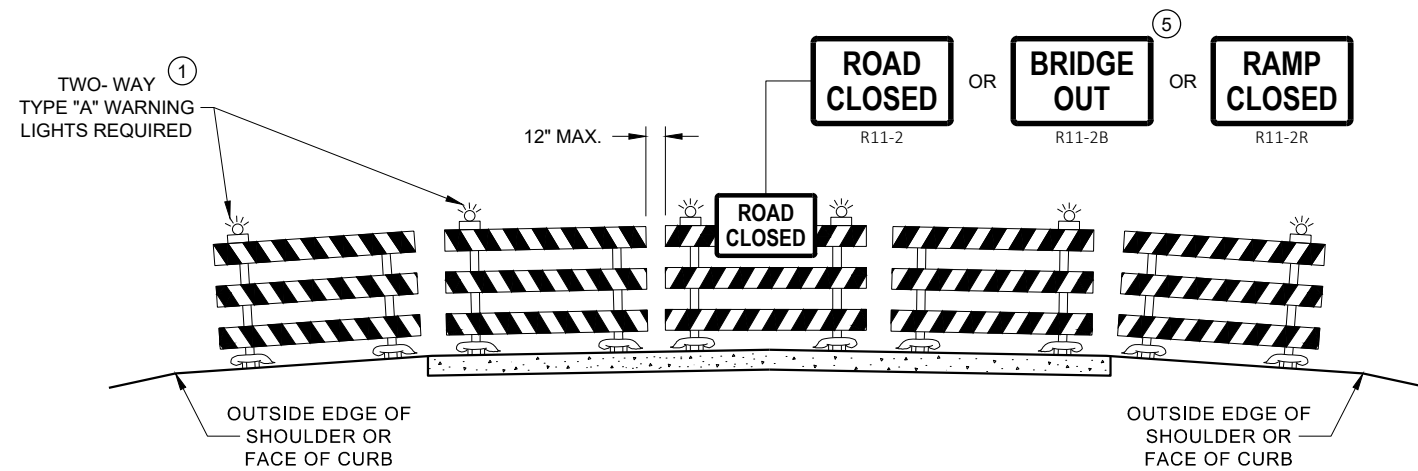


DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR

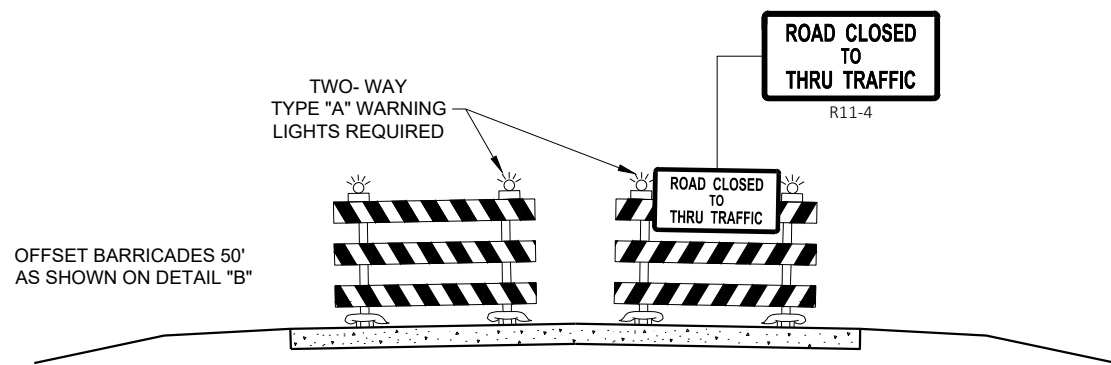
**BARRICADES AND SIGNS
 FOR MAINLINE CLOSURES**

STATE OF WISCONSIN
 DEPARTMENT OF TRANSPORTATION

APPROVED
 February 2020 /S/ Andrew Heidtke
 DATE DATE WORK ZONE ENGINEER
 FHWA



**DETAIL D
ROAD CLOSURE BARRICADE DETAIL
APPROACH VIEW**



**DETAIL E
LANE CLOSURE BARRICADE DETAIL
APPROACH VIEW**

SEE SDD 15C2 - SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW - INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11 - 2, R11 - 3, M4 - 9, R11 - 4, AND R10 - 61 SIGNS PLACED ON THE BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE RAIL OR BOTTOM RAILS.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

- R11 - 2 SHALL BE 48" X 30"
- R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60" X 30"
- M4 - 9 SHALL BE 30" X 24"
- M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1 - 1 SHALL BE 36" X 36"

- ① TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT SPACING).
- ② THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.
- ③ FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".
- ④ FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".
- ⑤ FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.
- ⑥ INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.
- ⑦ "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

**BARRICADES AND SIGNS
FOR
VARIOUS CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION




APPROVED
February 2020 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER
FHWA

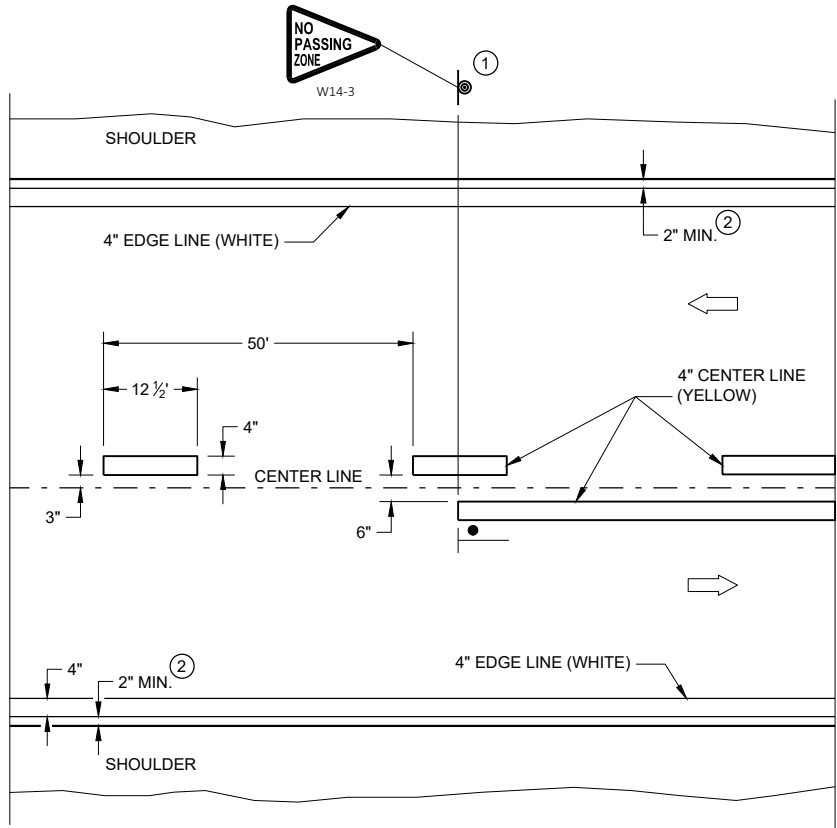
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

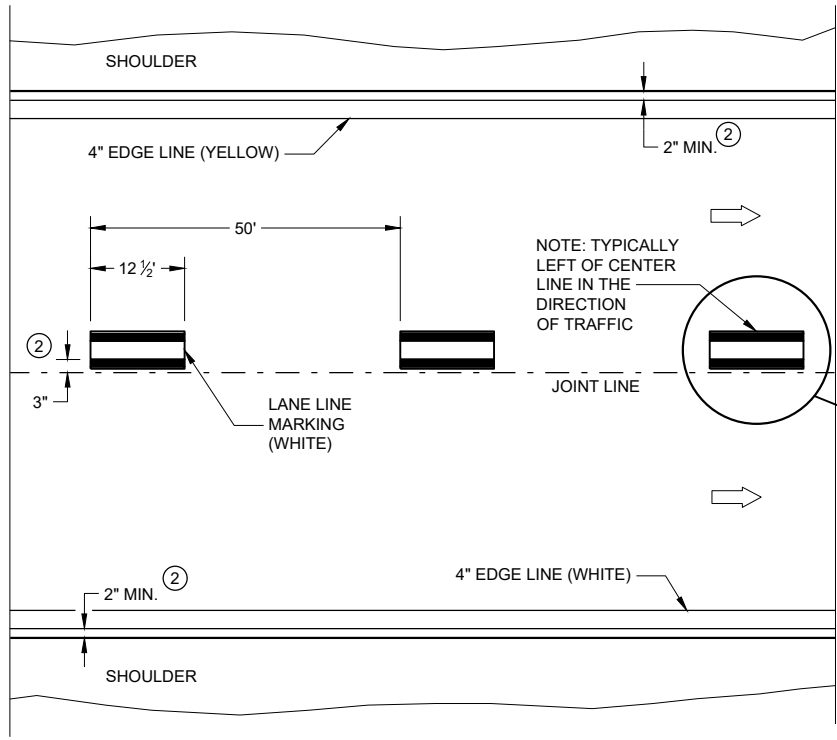
- ① LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING
- ② MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

LEGEND

-  "T" MARKING
-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC

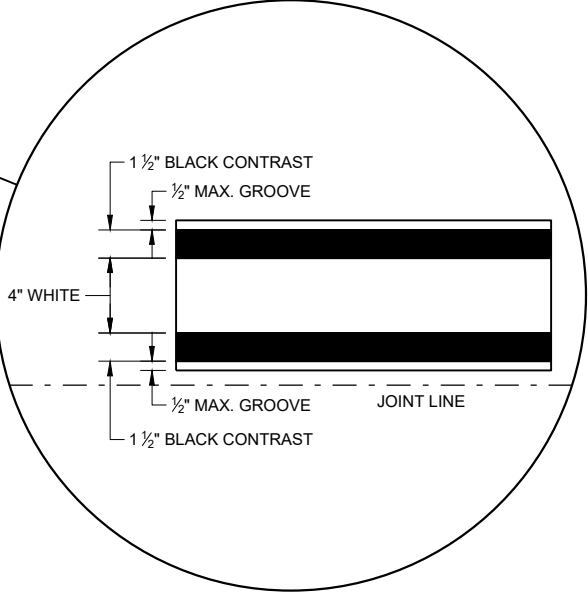


TWO WAY TRAFFIC



ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING



6

6

SDD 15C08 - 22a

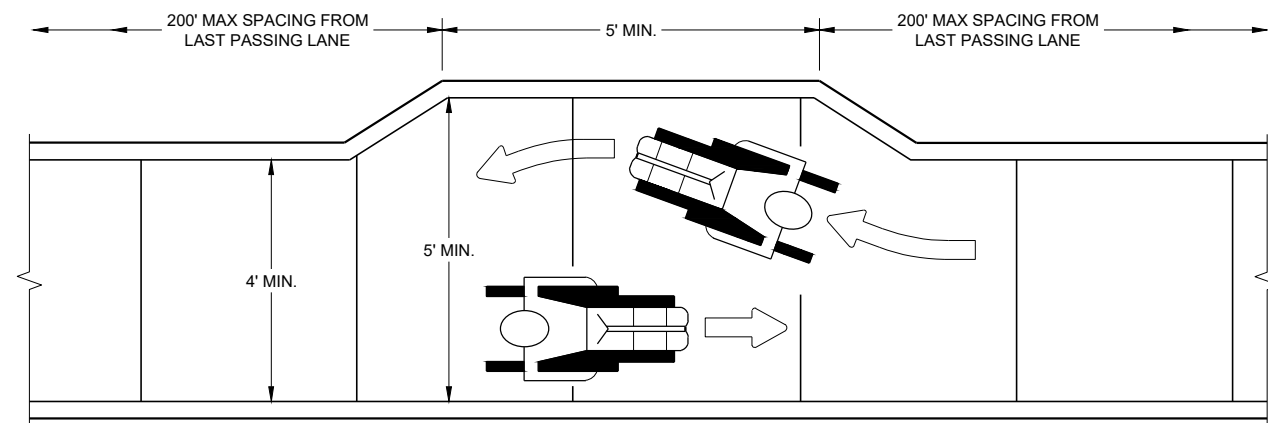
SDD 15C08 - 22a

PERMANENT LONGITUDINAL PAVEMENT MARKINGS

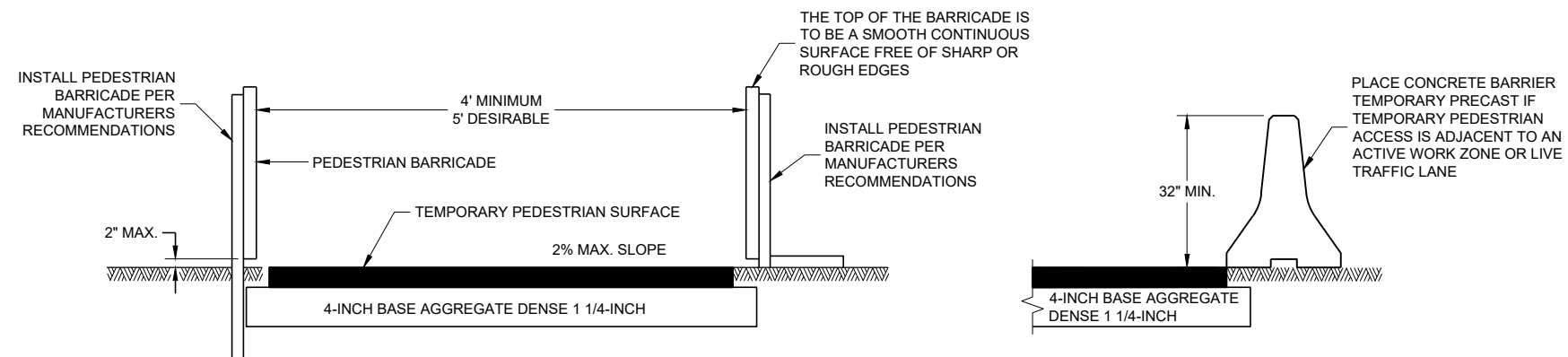
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE: May 2022 /S/ Jeannie Silver
STATEWIDE SIGNING AND MARKING ENGINEER

FHWA



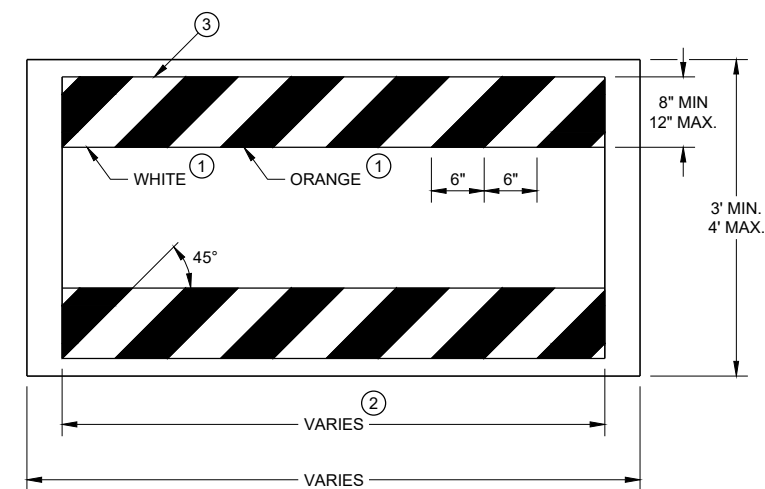
NARROW SIDEWALK PASSING DETAIL



TEMPORARY PEDESTRIAN ACCESS

GENERAL NOTES

- BARRICADE DEVICE SELECTED FROM APPROVED PRODUCT LIST
- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② SHEETING REQUIRED ON MORE THAN 50% OF BARRICADE WIDTH.
- ③ PLACE SHEETING ON BOTH SIDES OF THE BARRICADE.
- * USE THIS DETAIL FOR SHEETING PLACEMENT REFERENCE.

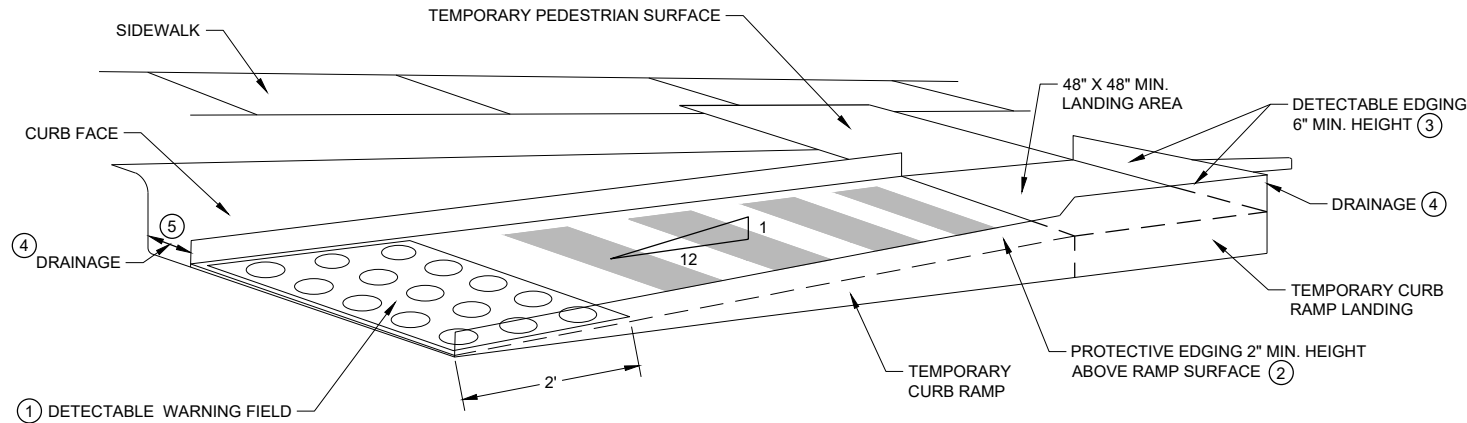


TEMPORARY PEDESTRIAN BARRICADE*

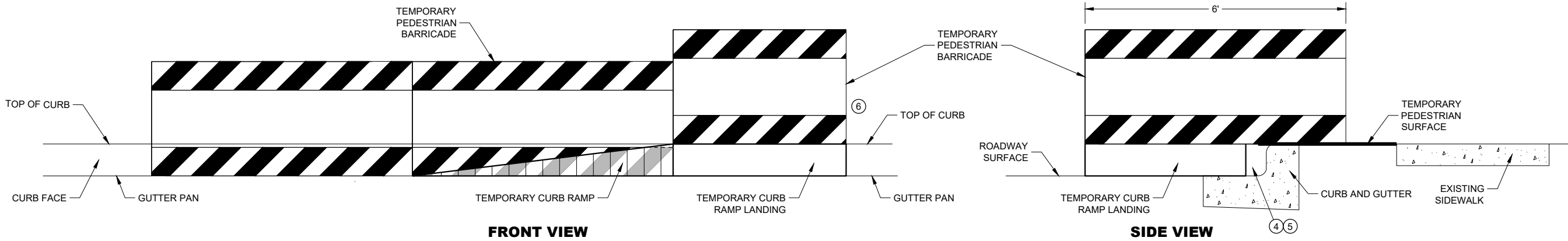
GENERAL NOTES

CURB RAMPS SHALL BE 48" MIN. WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE.
 CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.
 CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.
 LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.
 CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES MAY BE VERTICAL UP TO 1/4" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".

- ① INSTALL CONTRASTING TEMPORARY DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS, AS SHOWN IN THE PLANS.
- ② PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- ③ DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- ④ DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ⑤ ENSURE CURB RAMP IS OUT OF THE GUTTER PAN.
- ⑥ IF ONLY PART OF THE END PANEL OF TEMPORARY PEDESTRIAN BARRICADE PANEL IS NEEDED, EXTEND EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL HERE.



PERSPECTIVE VIEW

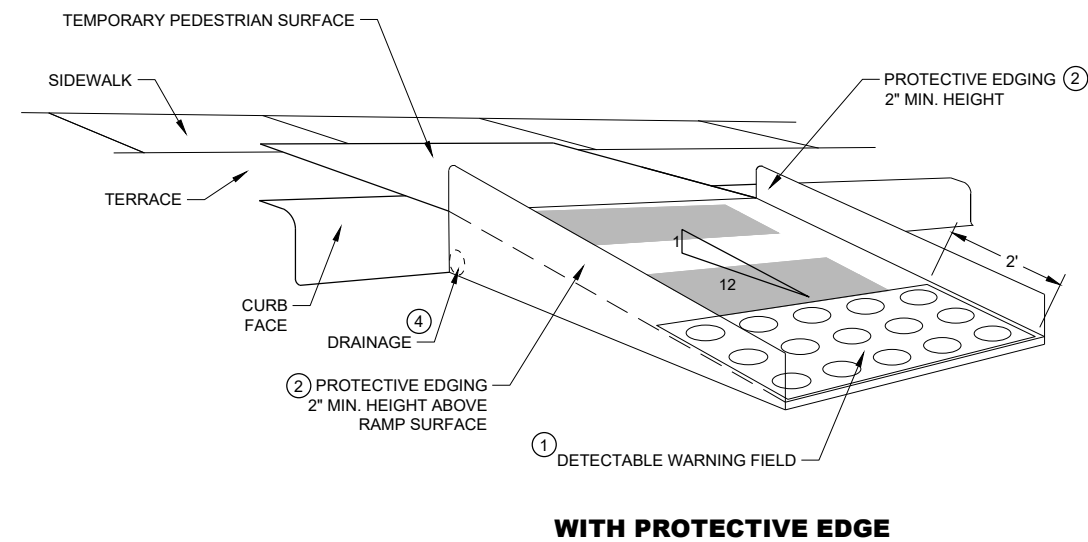
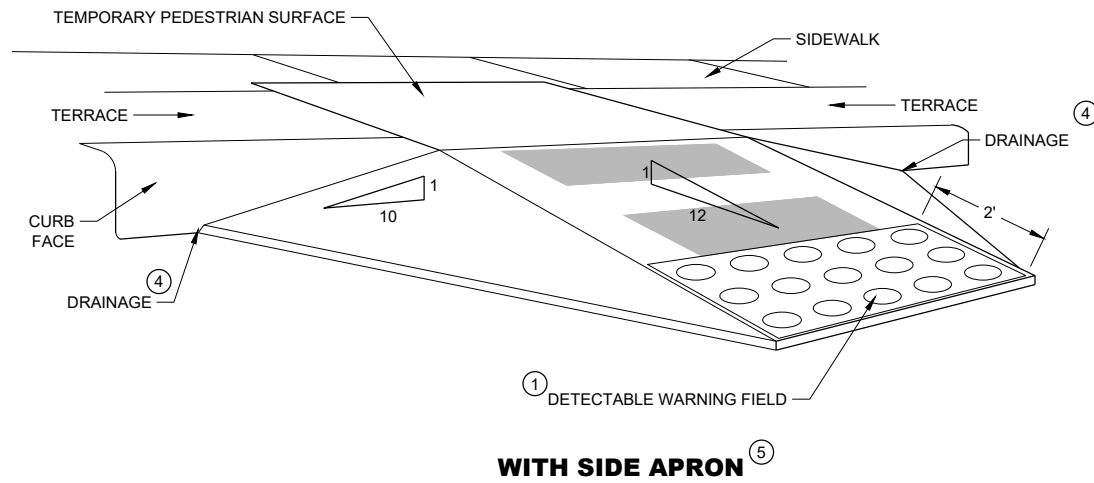


FRONT VIEW

SIDE VIEW

TEMPORARY CURB RAMP PARALLEL TO CURB

<p>TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION</p>
<p>STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION</p>



TEMPORARY CURB RAMP PERPENDICULAR TO CURB

GENERAL NOTES

CURB RAMPS SHALL BE 48" MINIMUM WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE.

ALTERNATE SIDEWALK WORK BETWEEN LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.

CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.







CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.

LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.

CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES MAY BE VERTICAL UP TO 1/4" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".

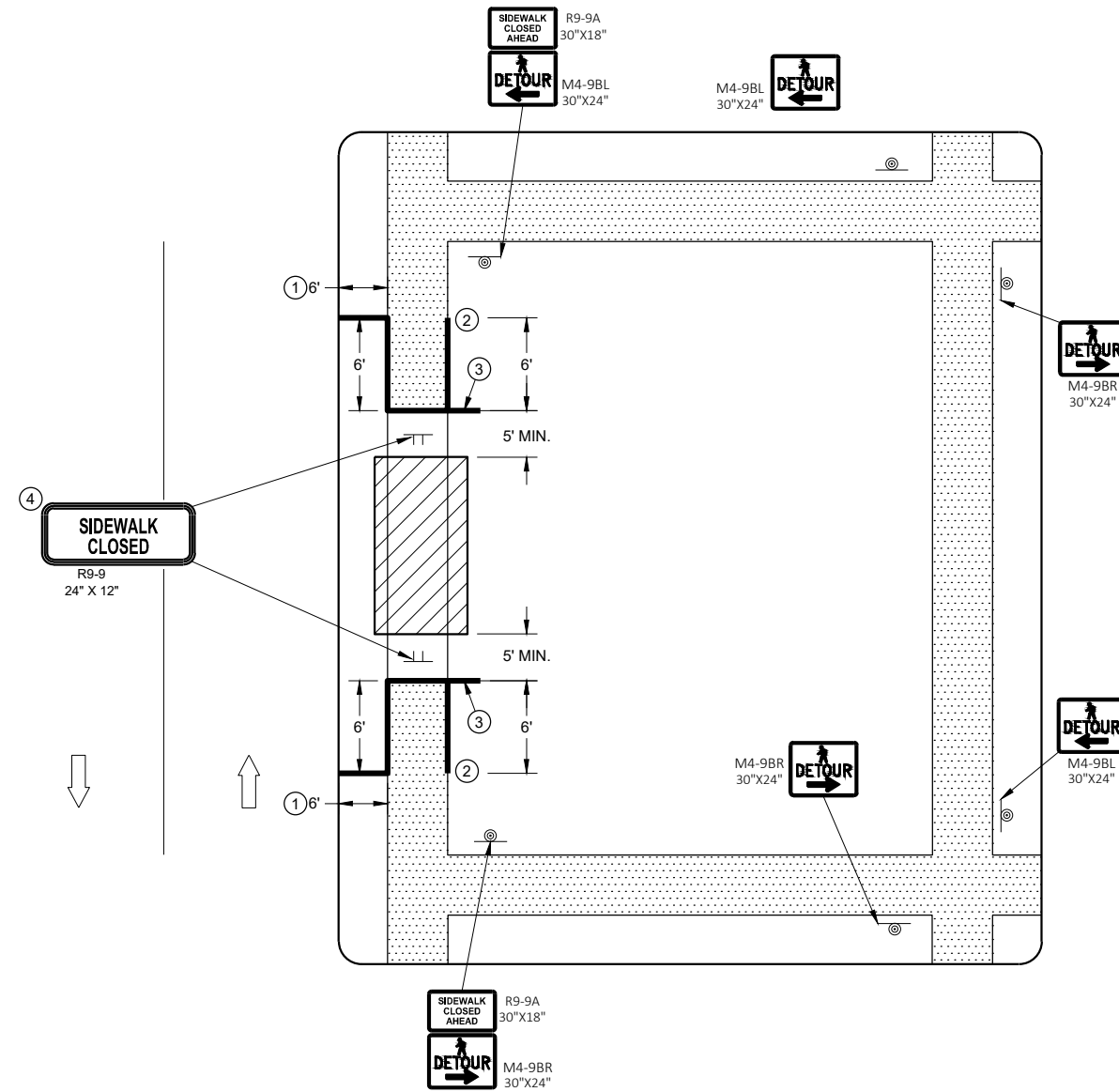
- ① INSTALL CONTRASTING TEMPORARY DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS, AS SHOWN IN THE PLANS
- ② PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- ③ DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- ④ DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ⑤ CAN ONLY BE USED FOR RAMPS WITH 6" OR LESS OF VERTICAL CHANGE.

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  SIGN ON TEMPORARY SUPPORT
-  UNDER PEDESTRIAN TRAFFIC
-  WORK AREA
-  TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC

GENERAL NOTES

- SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.
- WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.
- SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.
- PLACE TEMPORARY PEDESTRIAN BARRICADE TO FIT FIELD CONDITIONS, AVOIDING CONFLICTS WITH DRIVEWAYS AND OTHER EXISTING FEATURES.
- ① IF TERRACE IS LESS THAN 6 FEET WIDE, OMIT TEMPORARY PEDESTRIAN BARRICADE FROM THE SIDEWALK TO THE CURB.
 - ② PLACE BARRICADE CLOSURE SO THAT THE TEMPORARY PEDESTRIAN BARRICADE END IS AT THE LAST OPEN SIDEWALK ACCESS TO RESIDENCES OR BUSINESSES BEFORE THE SIDEWALK CLOSURE.
 - ③ IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
 - ④ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.

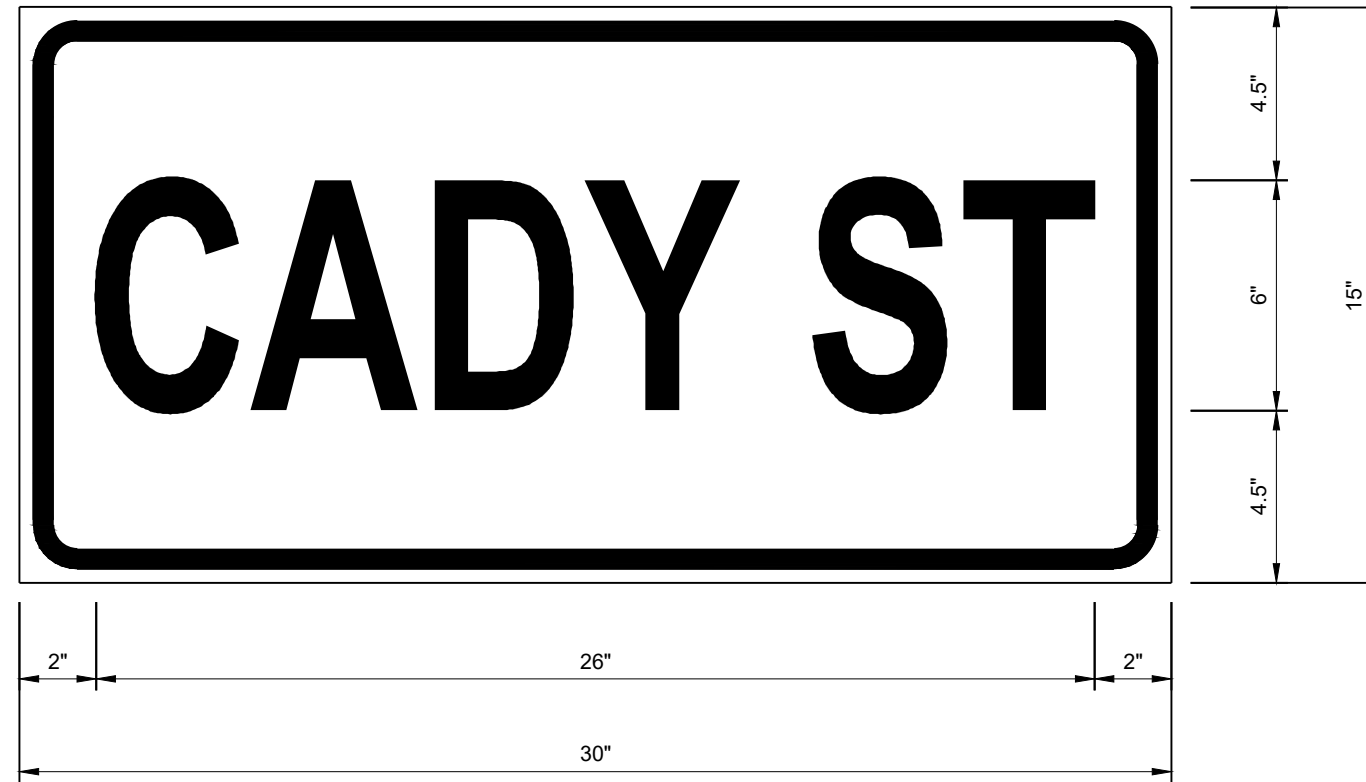


SIDEWALK DETOUR, SIDEWALK ONLY ON ONE SIDE

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

NOTES:

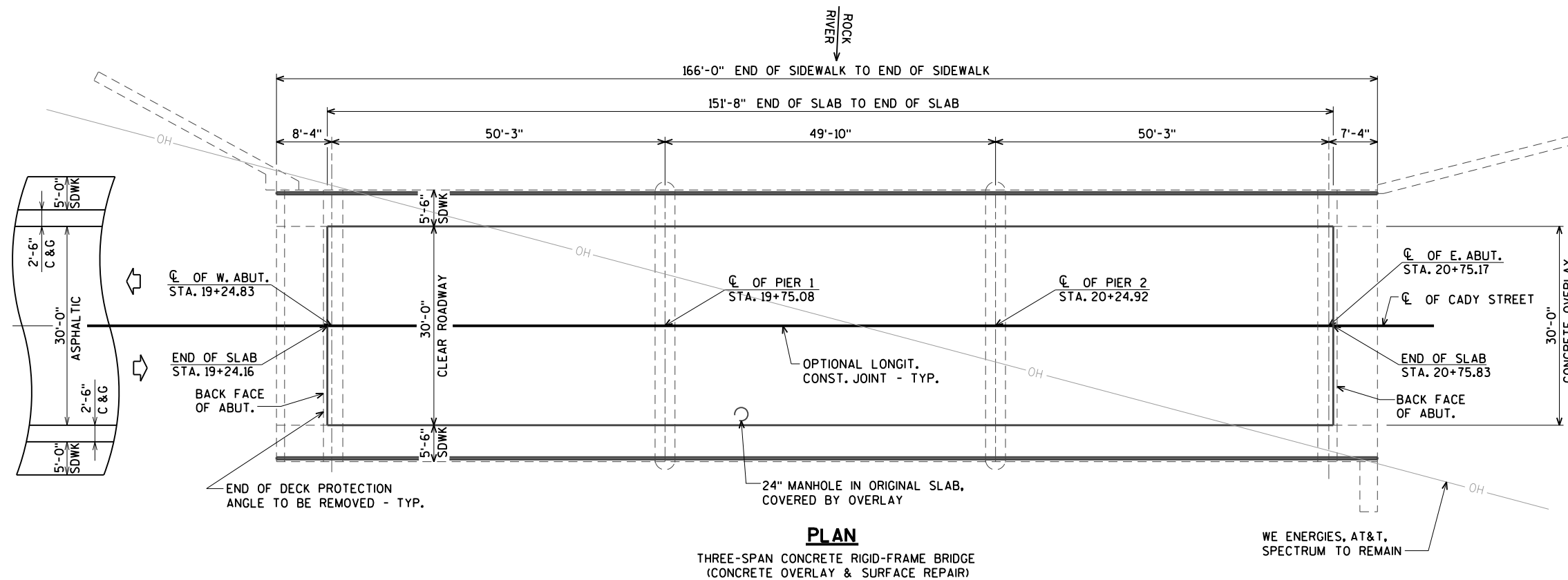
1. ALL SIGNS TYPE II - TYPE F REFLECTIVE
2. COLOR:
BACKGROUND - ORANGE
MESSAGE - BLACK
3. MESSAGE SERIES - C



1.125" Radius, 0.50" Boarder, 0.375" Indent

7

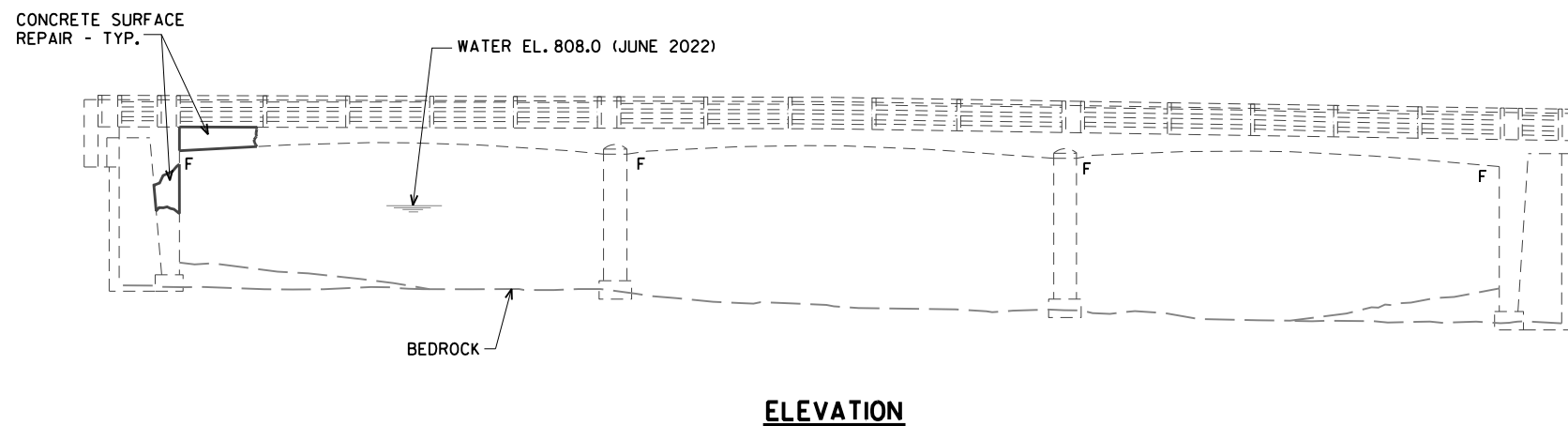
7



DATE: \$DATE\$
\$PEN\$

CHECKED BY: DATE: \$DATE\$
BACK CHECKED BY: DATE: \$DATE\$
CORRECTED BY:

8



NOTE:
CONCRETE SURFACE REPAIR
REQUIRED AS DIRECTED BY THE
ENGINEER IN THE FIELD.

LIST OF DRAWINGS

1. GENERAL PLAN
2. TYPICAL SECTION, QUANTITIES & NOTES

FOR TYPICAL SECTION,
DESIGN DATE & NOTES
SEE SHEET 2



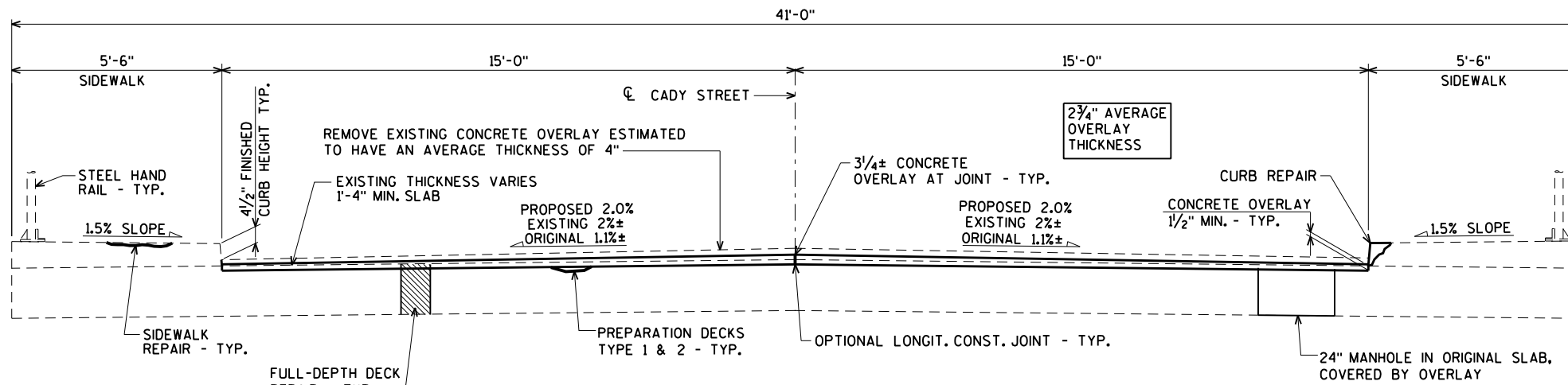
1/27/2023

BRIDGE OFFICE CONTACT:
AARON BONK
(608)-261-0261

CONSULTANT CONTACT:
KRISTOFER OLSON
(920)-498-1200

NO.	DATE	REVISION	BY
ORIGINAL PLANS PREPARED BY AVRES 3433 Oakwood Hills Parkway Eau Claire, WI 54701 www.AyresAssociates.com			
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
ACCEPTED	<i>[Signature]</i>	SDR	02/07/23 DATE
CHIEF STRUCTURES DESIGN ENGINEER			
STRUCTURE P-28-706			
CADY STREET OVER ROCK RIVER			
COUNTY	JEFFERSON	TOWN/CITY/VILLAGE	WATERTOWN
DESIGN SPEC. REHABILITATION N/A			
DESIGNED BY	KRO	DESIGN CK'D.	AEB
DRAWN BY	CLP	PLANS CK'D.	KRO
GENERAL PLAN			SHEET 1 OF 2

8



TYPICAL SECTION THRU BRIDGE

(LOOKING EAST)

TOTAL ESTIMATED QUANTITIES

BID ITEM NUMBER	BID ITEMS	UNIT	TOTAL
502.3200	PROTECTIVE SURFACE TREATMENT	SY	506
502.3215	PROTECTIVE SURFACE TREATMENT RESEAL	SY	205
509.0301	PREPARATION DECKS TYPE 1	SY	50
509.0302	PREPARATION DECKS TYPE 2	SY	25
509.0505.S	CLEANING DECKS TO REAPPLY CONCRETE MASONRY OVERLAY	SY	506
509.1200	CURB REPAIR	LF	50
509.1500	CONCRETE SURFACE REPAIR	SF	244
509.2000	FULL DEPTH DECK REPAIR	SY	1
509.2500	CONCRETE MASONRY OVERLAY DECKS	CY	42
509.9005.S	REMOVING CONCRETE MASONRY DECK OVERLAY P-28-706	SY	506
SPV.0060	REPLACEMENT MANHOLE COVER	EACH	1

▲ BID ITEM INCLUDES CONCRETE FOR 1/2" MIN. OVERLAY, PREPARATION DECKS TYPE 1, PREPARATION DECKS TYPE 2, AND FULL DEPTH DECK REPAIR.

⊙ UNDISTRIBUTED AS DIRECTED BY ENGINEER IN THE FIELD.

■ IF REQUIRED BY THE ENGINEER.

DESIGN DATA

LIVE LOAD:

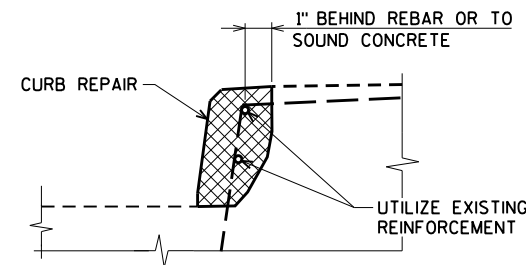
DESIGN LOADING: H-15
 INVENTORY RATING: HS-13
 OPERATING RATING: HS-21
 WISCONSIN STANDARD PERMIT BEHICLE (WIS-SPV): 230 KIPS

MATERIAL PROPERTIES:

CONCRETE MASONRY { DECK PATCHING & OVERLAY $f'_c = 4,000$ p.s.i.
 { ALL OTHER $f'_c = 3,500$ p.s.i.

TRAFFIC DATA:

CADY STREET
 A.A.D.T. = 4,200 (2023)
 A.A.D.T. = 4,200 (2043)
 R.D.S. = 30 M.P.H.



CURB REPAIR DETAIL

NOTE:
 LIMITS OF CURB REPAIR SHALL BE DETERMINED IN THE FIELD BY THE ENGINEER. VERTICAL AND HORIZONTAL LIMITS OF CURB REPAIR SHALL BE DEFINED BY A 1/2" DEEP SAW CUT.

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED. DIMENSIONS SHOWN ARE BASED ON THE ORIGINAL STRUCTURE PLANS. PROTECTIVE SURFACE TREATMENT SHALL BE APPLIED TO THE ENTIRE TOP SURFACE OF THE NEW CONCRETE OVERLAY. PROTECTIVE SURFACE TREATMENT RESEAL SHALL BE APPLIED TO THE ENTIRE TOP SURFACE OF THE EXISTING SIDEWALK AND FACE OF EXISTING CURB.

REMOVAL OF THE EXISTING CONCRETE OVERLAY AND THE END OF DECK PROTECTION ANGLE SHALL BE INCLUDED IN THE BID ITEM "REMOVING CONCRETE MASONRY DECK OVERLAY".

THE AVERAGE OVERLAY THICKNESS IS BASED ON THE MINIMUM OVERLAY THICKNESS PLUS 1/2-INCH TO ACCOUNT FOR VARIATIONS IN THE DECK SURFACE.

PREPARATION DECKS TYPE 1, PREPARATION DECKS TYPE 2, AND FULL-DEPTH DECK REPAIR AREAS ARE BASED ON THE PLANS AND AS DETERMINED BY THE ENGINEER. DECK PREPARATION AND FULL-DEPTH DECK REPAIRS SHALL BE FILLED WITH "CONCRETE MASONRY OVERLAY DECKS".

ANY EXCAVATION REQUIRED TO COMPLETE THE OVERLAY AT THE ABUTMENTS TO BE CONSIDERED INCIDENTAL TO THE BID ITEM "CONCRETE MASONRY OVERLAY DECKS".

PROFILE GRADE LINE SHALL BE DETERMINED IN THE FIELD BASED ON A MINIMUM OVERLAY THICKNESS OF 1/2" PLACED ABOVE THE DECK SURFACE AFTER SURFACE PREPARATION. EXPECTED AVERAGE OVERLAY THICKNESS IS 2 3/4". IF EXPECTED AVERAGE OVERLAY THICKNESS IS EXCEEDED BY MORE THAN 1/2", CONTACT THE STRUCTURES DESIGN SECTION.

ALL CONCRETE REMOVAL NOT COVERED WITH A CONCRETE OVERLAY SHALL BE DEFINED BY A 1" DEEP SAW CUT.

ALL PREVIOUS PATCHES SHALL BE REMOVED UNDER THE BID ITEM "PREPARATION DECKS".

UTILIZE EXISTING BAR STEEL REINFORCEMENT WHERE SHOWN AND EXTEND 24 BAR DIAMETERS INTO NEW WORK.

BEVEL EXPOSED EDGES OF CONCRETE 3/4" UNLESS NOTED OTHERWISE.

SEAL OVERLAY CONSTRUCTION JOINTS ACCORDING TO SECTION 502.3.13.1 OF THE STANDARD SPECIFICATIONS. COST INCIDENTAL TO BID ITEM "CONCRETE MASONRY OVERLAY DECKS".

"CONCRETE SURFACE REPAIR" TO INCLUDE BUT NOT LIMITED TO ABUTMENTS, SIDEWALKS, AND EDGE OF DECK AS DIRECTED BY THE ENGINEER.

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STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE P-28-706			
DRAWN BY		CLP	PLANS CK'D. KRO
TYPICAL SECTION, QUANTITIES & NOTES			SHEET 2 OF 2

ORIGINAL PLANS PREPARED BY
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