

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
5540-02-71	WISC 2024022	1

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

PRAIRIE DU CHIEN - VIROQUA

0.1 MI N NORTH AVENUE TO CTH C

STH 27

CRAWFORD COUNTY

STATE PROJECT NUMBER
5540-02-71

ORDER OF SHEETS

Section No. 1	Title
Section No. 2	Typical Sections and Details
Section No. 3	Estimate of Quantities
Section No. 3	Miscellaneous Quantities
Section No. 4	Right of Way Plat
Section No. 5	Plan and Profile
Section No. 6	Standard Detail Drawings
Section No. 7	Sign Plates
Section No. 8	Structure Plans
Section No. 9	Computer Earthwork Data
Section No. 9	Cross Sections

TOTAL SHEETS = 170



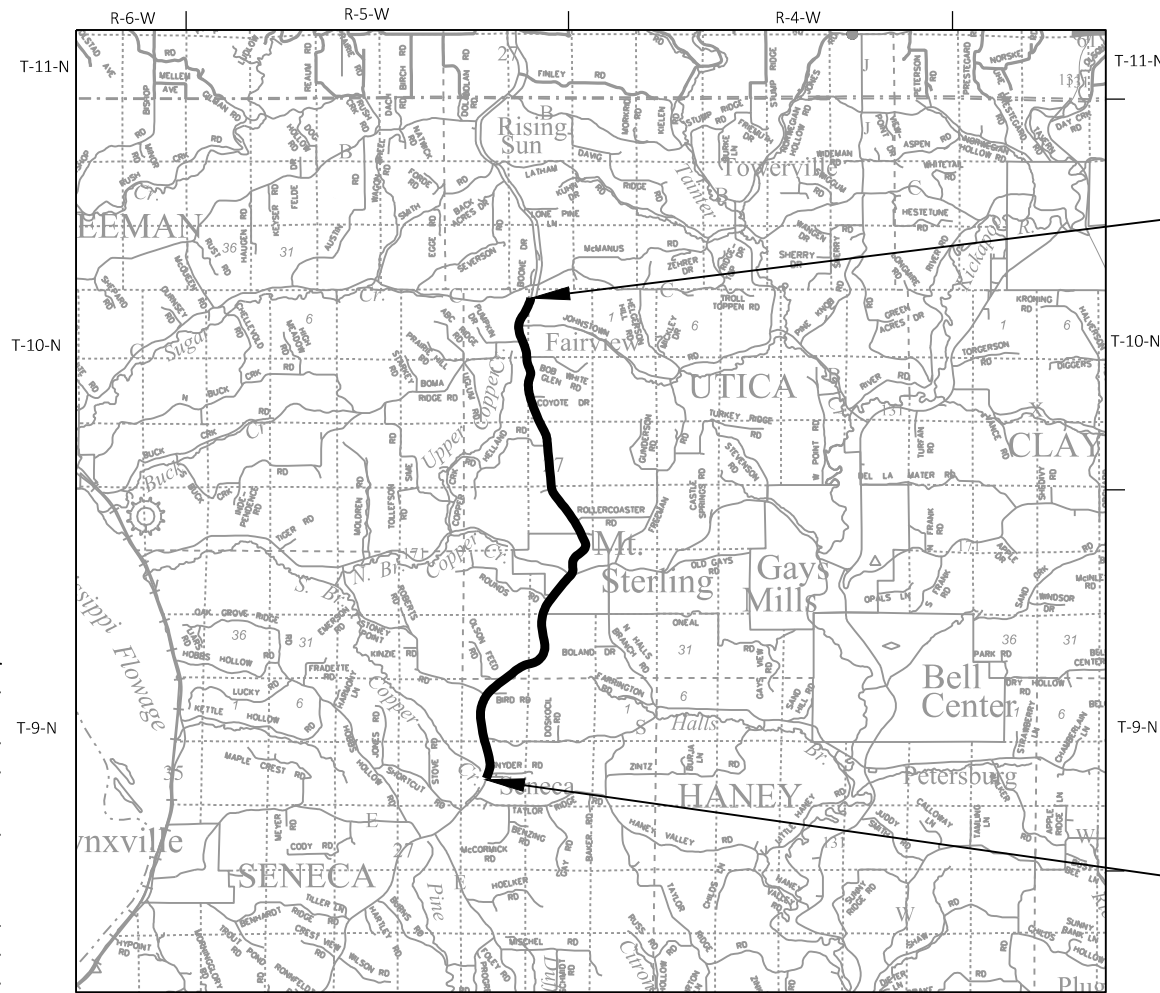
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DESIGN DESIGNATION 5540-02-01

A.A.D.T. 2024	=	1,300
A.A.D.T. 2044	=	1,600
D.H.V.	=	190
D.D.	=	60/40
T.	=	18.2%
DESIGN SPEED	=	35 MPH - 60 MPH
ESALS	=	430,000

CONVENTIONAL SYMBOLS

PLAN	PROFILE
CORPORATE LIMITS	GRADE LINE
PROPERTY LINE	ORIGINAL GROUND
LOT LINE	MARSH OR ROCK PROFILE (To be noted as such)
LIMITED HIGHWAY EASEMENT	SPECIAL DITCH
EXISTING RIGHT OF WAY	GRADE ELEVATION
PROPOSED OR NEW R/W LINE	CULVERT (Profile View)
SLOPE INTERCEPT	UTILITIES
REFERENCE LINE	ELECTRIC
EXISTING CULVERT	FIBER OPTIC
PROPOSED CULVERT (Box or Pipe)	GAS
COMBUSTIBLE FLUIDS	SANITARY SEWER
	STORM SEWER
	TELEPHONE
MARSH AREA	WATER
	UTILITY PEDESTAL
	POWER POLE
WOODED OR SHRUB AREA	TELEPHONE POLE



END PROJECT
STA 1423+49.0

BEGIN PROJECT
STA 979+47.3
Y=200,928.975
X=368,603.739

LAYOUT
SCALE 0 3 MI
TOTAL NET LENGTH OF CENTERLINE = 8.409 MILES

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS), CRAWFORD COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES. ELEVATIONS ARE REFERENCED TO NAVD 88 (2012). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 12A.

ACCEPTED FOR
VILLAGE OF MT. STERLING
Date: 7/17/23
(Signature and Title of Official)

ORIGINAL PLANS PREPARED BY
JT ENGINEERING, INC
CONSULTANT SERVICES
BRADLEY R. GROH
E-41411
MADISON WI
PROFESSIONAL ENGINEER
DATE: 7/17/2023
(Professional Engineer Signature)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
PREPARED BY
Surveyor: WISDOT & JT ENGINEERING
Designer: JT ENGINEERING, INC
Project Manager: TERI SCHOPP, P.E.
Regional Examiner: SW REGION
Regional Supervisor: JOHN BAINTEK, P.E.

APPROVED FOR THE DEPARTMENT
DATE: 7/17/2023
(Signature)

E

PROJECT ID: 5540-02-71

COUNTY: CRAWFORD

GENERAL NOTES

IF THERE ARE UTILITY FACILITIES WITHIN THE PROJECT AREA THAT ARE NOT SHOWN ON THE PLANS, THE CONTRACTOR SHALL COORDINATE THEIR CONSTRUCTION ACTIVITIES WITH A CALL TO DIGGER'S HOTLINE AND/OR A DIRECT CALL TO THE UTILITIES THAT HAVE FACILITIES IN THE AREA.

WHEN THE QUANTITY OF BASE AGGREGATE IS MEASURED BY THE TON OR CUBIC YARD, THE DEPTH OR THICKNESS OF THE LAYER SHOWN ON THE PLANS IS APPROXIMATE AND THE ACTUAL THICKNESS WILL DEPEND ON THE DISTRIBUTION OF THE MATERIAL AS DIRECTED BY THE ENGINEER.

INSTALL WISDOT URBAN TYPE A EROSION CONTROL MAT ON ALL DISTURBED AREAS WITHIN THE VILLAGE OF MT. STERLING.

APPLY TACK COAT AT A RATE OF 0.07 GAL/SY TO MILLED PAVEMENT SURFACES AND 0.05 GAL/SY BETWEEN LAYERS OF HMA PAVEMENT.

HMA PAVEMENT WEIGHT CALCULATIONS ARE BASED ON 112 LB/SY/IN.

THE CONTRACTOR'S PAVING OPERATIONS SHALL BE CONSISTENT WITH THE PLAN TYPICAL SECTIONS AND CONSTRUCTED TO PREVENT HMA LONGITUDINAL JOINTS FROM BEING LOCATED WITHIN A DRIVING, TURNING, PASSING, OR PARKING LANE.

RIGHT OF WAY LINES SHOWN IN THE PLANS ARE APPROXIMATE.

CONTRACTORS SHALL MAINTAIN ACCESS TO ALL DRIVEWAYS AT ALL TIMES. ACCESS SHALL BE PROVIDED DURING NON-WORKING HOURS.

NUMBER, LOCATION, AND SPACING OF SIGNS AND DEVICES, AS SHOWN PLAN SHALL BE ADJUSTED TO FIELD CONDITIONS.

DISTURBED AREAS WITHIN THE RIGHT OF WAY ARE TO BE TOPSOILED, FERTILIZED, SEEDED AND MULCHED AS DIRECTED AS BY THE ENGINEER.

VERIFY LOCATIONS OF BEAM GUARD ITEMS WITH ENGINEER.

SECTION 2 ORDER OF SHEETS

- GENERAL NOTES
- PROJECT OVERVIEW
- TYPICAL SECTIONS
- CONSTRUCTION DETAILS
- CURB RAMP DETAILS
- EROSION CONTROL
- PAVEMENT MARKINGS
- TRAFFIC CONTROL

UTILITY CONTACTS

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ALLIANT ENERGY - ELECTRICITY
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EMAIL: WPLROADPLANS@ALLIANTENERGY.COM

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2489 RINDEN ROAD
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MADISON, WI 53701
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MT. STERLING, WI 54645
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CHAD OLMSTEAD
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22439 STATE HIGHWAY 35
LYNXVILLE, WI 54626
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JAY GRIMSLED
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WISDOT CONTACT

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JOHNSON CREEK, WI 53038
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CONSULTANT CONTACT

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BRADG@JT-ENGINEERING.COM

WISCONSIN DNR LIAISON

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(608) 406-7880
KAREN.KALVELAGE@WISCONSIN.GOV

WISCONSIN DOT RWIS PROGRAM

MIKE ADAMS
WISCONSIN DOT RWIS PROGRAM - COMM. TOWER
4822 MADISON YARDS WAY
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EMAIL: MICHAEL.ADAMS@DOT.WI.GOV



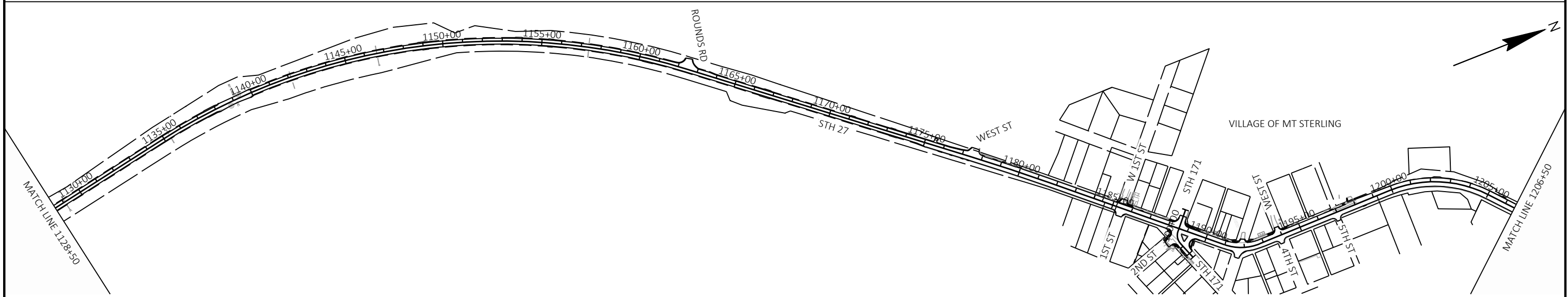
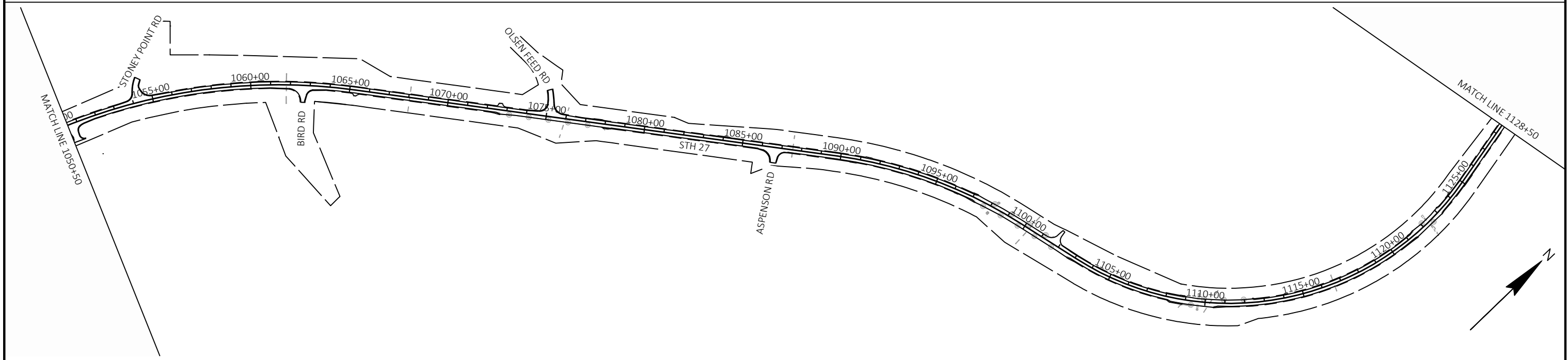
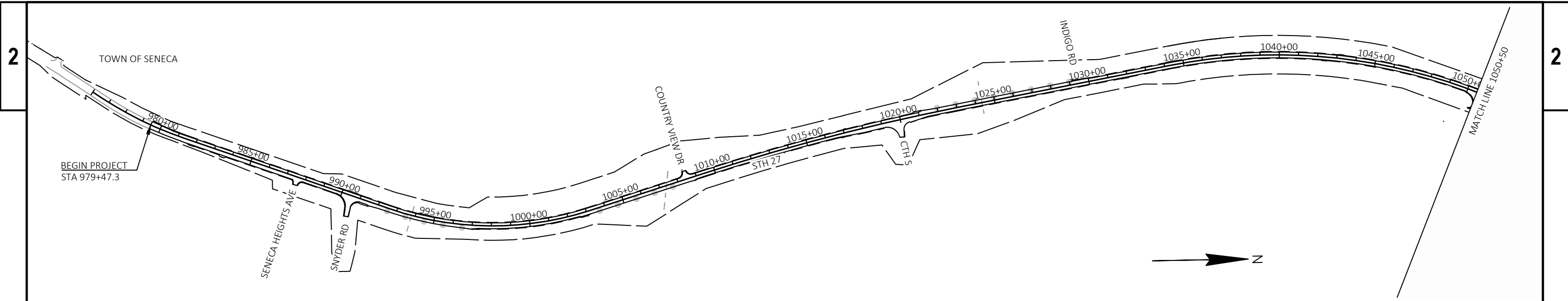
STANDARD ABBREVIATIONS

AC	ACRE	MP	MARKER POST
AGG	AGGREGATE	MGAL	1000 GALLONS
<	ANGLE	NC	NORMAL CROWN
AE, AEW	APRON ENDWALL	N	NORTH
ASPH	ASPHALTIC	NB	NORTH BOUND
ADT	AVERAGE DAILY TRAFFIC	NOR	NORMAL
AADT	ANNUAL AVERAGE DAILY TRAFFIC	NO	NUMBER
BAD	BASE AGGREGATE DENSE	PAV'T	PAVEMENT
BF	BACK FACE	PLE	PERMANENT LIMITED EASEMENT
BM	BENCHMARK	PC	POINT OF CURVATURE
BTWN	BETWEEN	PI	POINT OF INTERSECTION
CTR	CENTER	PT	POINT OF TANGENCY
C/L	CENTERLINE	PCC	PORTLAND CEMENT CONCRETE
Δ	CENTRAL ANGLE OR DELTA	PE	PRIVATE ENTRANCE
CE	COMMERCIAL ENTRANCE	PGL	PROFILE GRADE LINE
CONST	CONSTRUCTION	PL	PROPERTY LINE
CMCP	CORRUGATED METAL CULVERT PIPE	R	RADIUS OR RANGE
CMP	CORRUGATED METAL PIPE	R/L	REFERENCE LINE
CO	COUNTY	RCCP	REINFORCED CONCRETE CULVERT PIPE
CTH	COUNTY TRUNK HIGHWAYS	REQ'D	REQUIRED
CY	CUBIC YARD	RT	RIGHT
CP	CONTROL POINT OR CULVERT PIPE	R/W	RIGHT OF WAY
C&G	CURB AND GUTTER	RD	ROAD
D	DEGREE OF CURVE	SHLD	SHOULDER(S)
DHV	DESIGN HOUR VOLUME	S	SOUTH
DIA	DIAMETER	SB	SOUTHBOUND
DD	DIRECTIONAL DISTRIBUTION	SF	SQUARE FOOT (FEET)
DISCH	DISCHARGE	SDD	STANDARD DETAIL DRAWING(S)
DMS	DYNAMIC MESSAGE SIGN	STH	STATE TRUNK HIGHWAYS
EA	EACH	SS	STORM SEWER
E	EAST	SSD	STOPPING SIGHT DISTANCE
EB	EASTBOUND	STA	STATION
ELEC	ELECTRIC(AL), ELEC. CABLE	SE	SUPERELEVATION
EL, ELEV	ELEVATION	S/L	SURVEY LINE
ESALS	EQUIVALENT SINGLE AXLE LOADS	SYM	SYMMETRICAL
EXC	EXCAVATION	T	TRUCKS (PERCENT OF)
EXIST	EXISTING	TEL	TELEPHONE
FF	FACE TO FACE	TEMP	TEMPORARY
FERT	FERTILIZER	TLE	TEMPORARY LIMITED EASEMENT
FE	FIELD ENTRANCE	TOC	TOP OF CURB
F/L, FL	FLOW LINE	TYP	TYPICAL
GALV	GALVANIZE	UNCL	UNCLASSIFIED
HS	HIGH STRENGTH	UG	UNDERGROUND
CWT	HUNDRED WEIGHT	VAR	VARIABLE
INL	INLET	VC	VERTICAL CURVE
INTER	INTERSECTION	VPC	VERTICAL POINT OF CURVATURE
JT	JOINT	VPI	VERTICAL POINT OF INTERSECTION
LT	LEFT	VPT	VERTICAL POINT OF TANGENCY
L	LENGTH OF CURVE	WT	WEIGHT
LF	LINEAR FOOT (FEET)	W	WEST
LC	LONG CHORD	WB	WEST BOUND
LS	LUMP SUM		

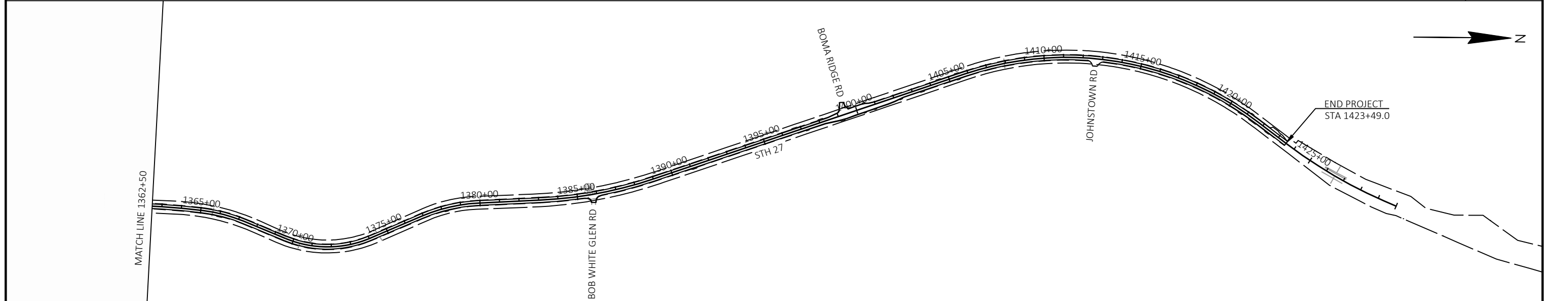
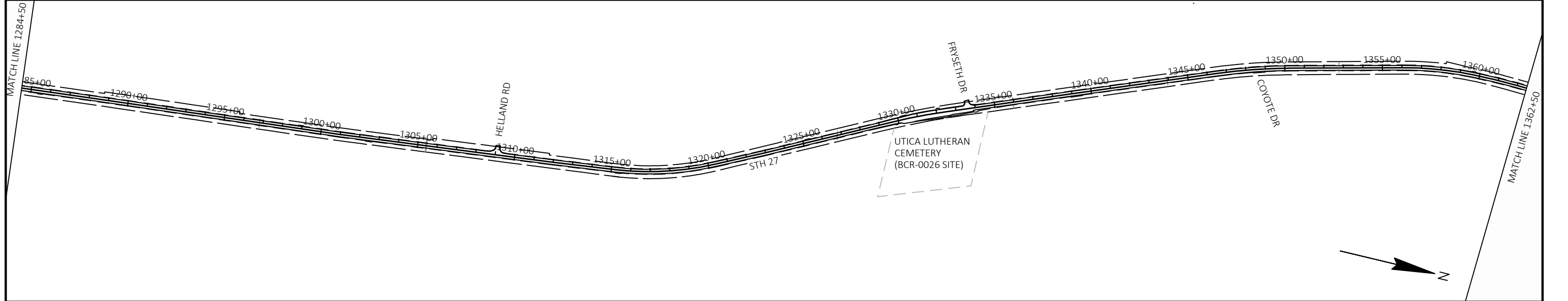
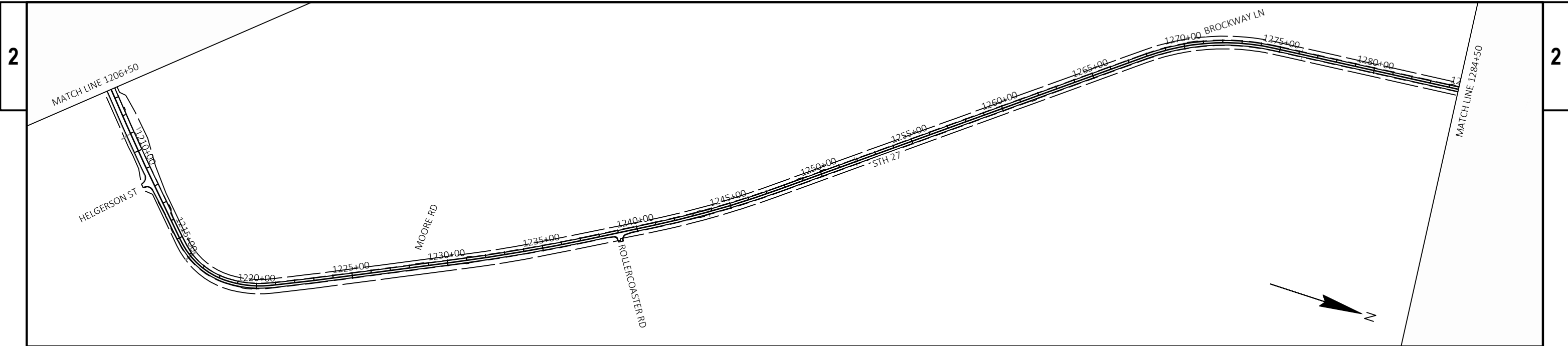
RUNOFF COEFFICIENT TABLE

	HYDROLOGIC SOIL GROUP											
	A			B			C			D		
	SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)		
LAND USE:	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER
ROW CROPS	.08 .22	.16 .30	.22 .38	.12 .26	.20 .34	.27 .44	.15 .30	.24 .37	.33 .50	.19 .34	.28 .41	.38 .56
MEDIAN STRIP-TURF	.19 .24	.20 .26	.24 .30	.19 .25	.22 .28	.26 .33	.20 .26	.23 .30	.30 .37	.20 .27	.25 .32	.30 .40
SIDE SLOPE-TURF			.25 .32			.27 .34			.28 .36			.30 .38
PAVEMENT:												
ASPHALT	.70 - .95											
CONCRETE	.80 - .95											
BRICK	.70 - .80											
DRIVES, WALKS	.75 - .85											
ROOFS	.75 - .95											
GRAVEL ROADS, SHOULDERS	.40 - .60											

TOTAL PROJECT AREA = .126.750 ACRES
 TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = .0412 ACRES



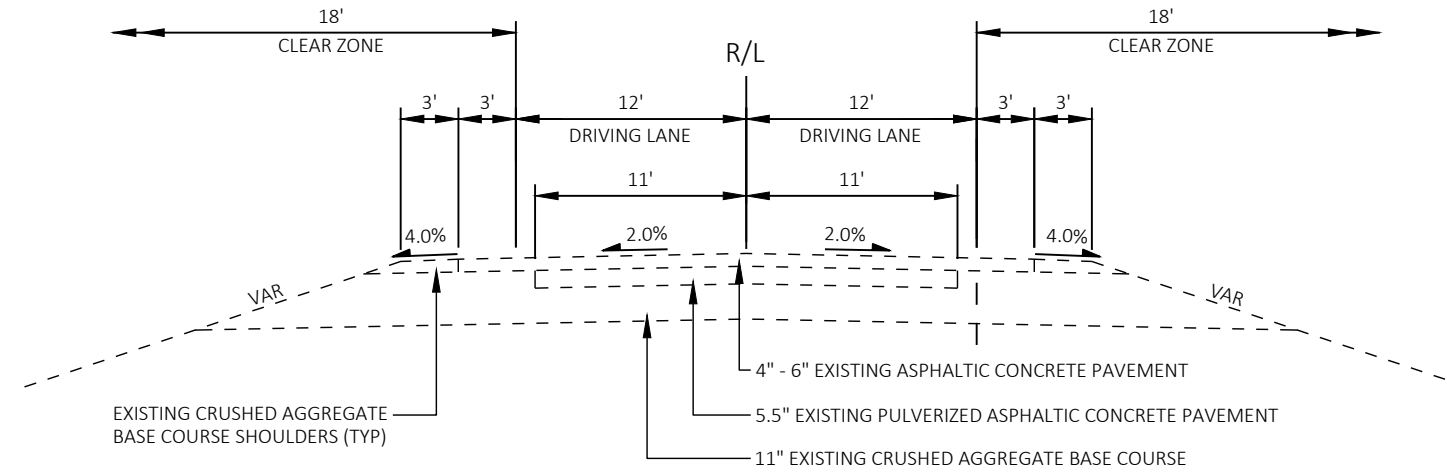
PROJECT NO: 5540-02-71	HWY: STH 27	COUNTY: CRAWFORD	PROJECT OVERVIEW	SHEET	E
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PROJECT NO: 5540-02-71	HWY: STH 27	COUNTY: CRAWFORD	PROJECT OVERVIEW	SHEET	E
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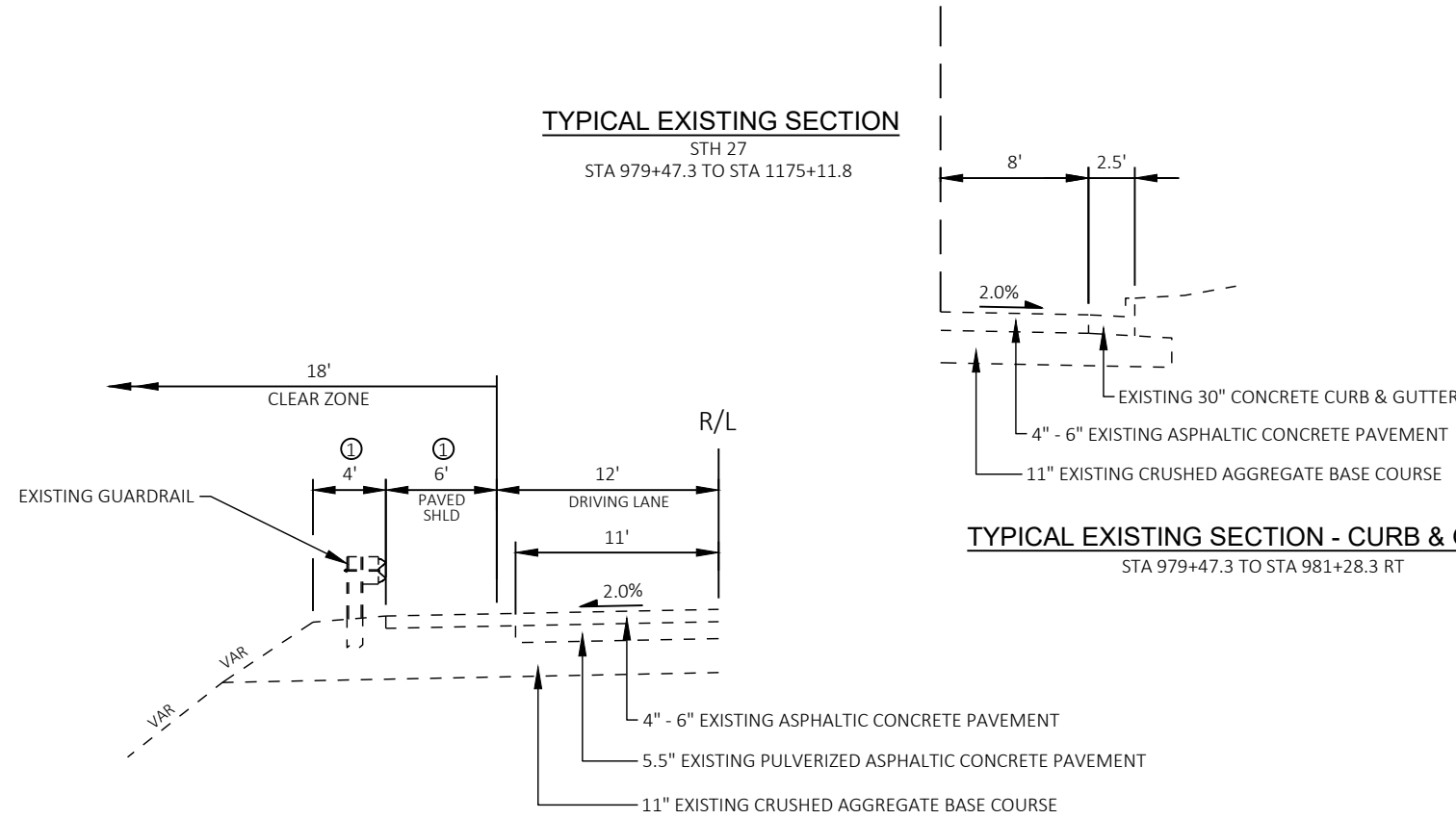
PAVEMENT BORING LOG					
BORING NUMBER	STATION	OFFSET	EXISTING ASPHALT DEPTH (INCHES)	BASE AGGREGATE DEPTH (INCHES)	TOTAL PAVEMENT STRUCUTRE (INCHES)
NORTH AVE					
B-1		9' RT	6"	11"	17"
B-2		3' RT	4"	9" RAP/5" BAD	18"
B-3		12' RT	5"	7" BAD/RAP/15" BAD	27"
CTH S					
B-4	1020+00				
B-4	1028+45	6' RT	5"	7" RAP/8" BAD	20"
B-5	1044+82	9' LT	4"	9" RAP/BAD	13"
B-6	1063+82	3' LT	5"	8" RAP/3" BAD	16"
B-7	1080+19	6' LT	4"	7" RAP/7" BAD	18"
B-8	1097+09	13' LT	6"	14" BAD	20"
B-9	1116+10	6' RT	4"	10" RAP/6" BAD	20"
B-10	1134+58	12' RT	5"	9" RAP/BAD/8" BAD	22"
B-11	1156+22	3' RT	4"	10" RAP/6" BAD	20"
ROUNDS RD					
B-12	1162+50				
B-12	1166+20	9' RT	3"	10" RAP/6" BAD	19"
B-13	1178+34	3' RT	5"	14" BAD/6" HMA/11" BAD	36"
STH 171					
B-14	1188+55				
B-14	1199+64	15' LT	5"	11" BAD	16"
B-15	1204+39	6' LT	4"	14" RAP/4" BAD	22"
B-16	1203+33	12' LT	5"	8" BAD	13"
ROLLERCOASTER RD					
B-17	1239+00				
B-17	1245+34	3' LT	12"	5" BAD	17"
B-18	1266+46	9' LT	15"	3" BAD	18"
B-19	1282+82	6' RT	10"	5" BAD	15"
B-20	1295+50	12' RT	5"	10" BAD	15"
B-21	1308+70	3' RT	9"	6" BAD	15"
HELLAND RD					
B-22	1309+00				
B-22	1322+73	9' RT	10"	9" BAD	19"
B-23	1336+46	9' LT	12"	12" BAD	24"
B-24	1353+88	3' LT	11"	3" RAP/5" BAD	19"
B-25	1364+44	12' LT	9"	4" RAP/6" BAD	19"
B-26	1377+11	6' RT	9"	10" BAD	19"
BOB WHITE GLEN RD					
B-27	1386+00				
B-27	1387+06	6' LT	9"	9" BAD	18"
B-28	1397+62	3' RT	11"	7" BAD	18"
B-29	1416+62	12' RT	4"	4" RAP/8" BAD	16"
B-30	1422+96	6' RT	4"	14" BAD	18"

NOTE: THE EXISTING ASPHALT DEPTHS MAY INCLUDE RECLAIMED ASPHALTIC PAVEMENT MATERIAL AS PART OF THE PAVEMENT STRUCTURE.



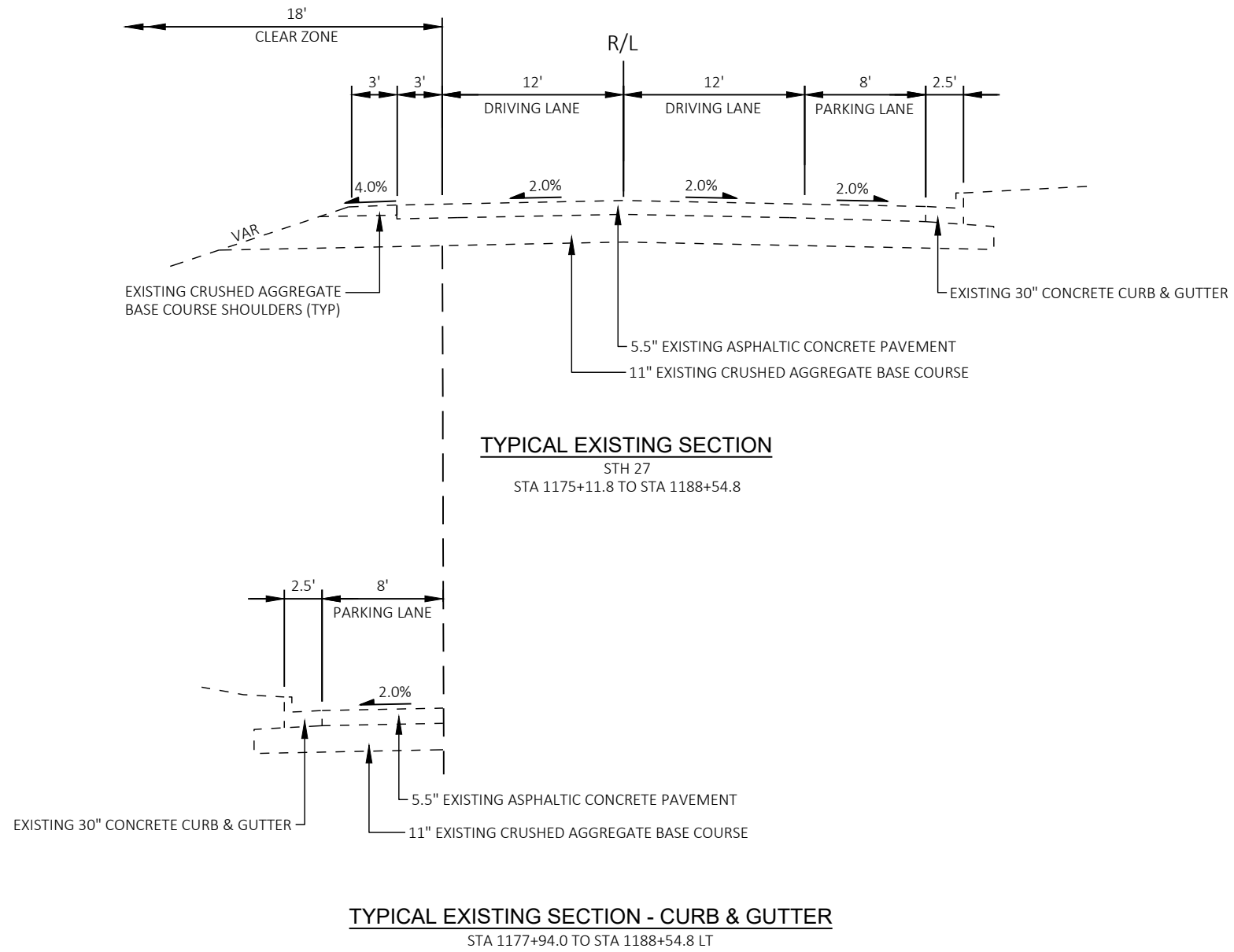
NOTES:
 ① WIDTH VARIES AT BEAM GUARD EAT LOCATIONS.

TYPICAL EXISTING SECTION
 STH 27
 STA 979+47.3 TO STA 1175+11.8



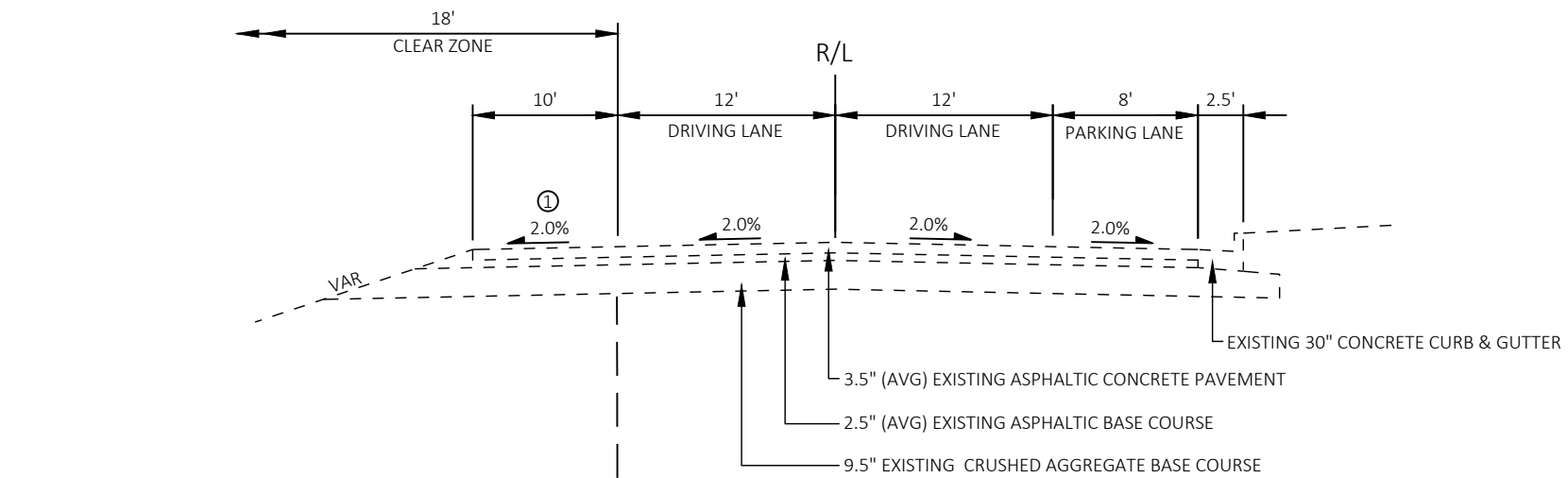
TYPICAL EXISTING SECTION - CURB & GUTTER
 STA 979+47.3 TO STA 981+28.3 RT

TYPICAL EXISTING SECTION - BEAM GUARD
 (TYPICAL REPRESENTS BOTH LEFT AND RIGHT SIDES)
 STH 27
 STA 991+50.0 TO STA 996+99.7 RT
 STA 1003+01.2 TO STA 1008+88.0 RT
 STA 1021+15.5 TO STA 1029+18.1 LT
 STA 1072+67.9 TO STA 1078+03.5 RT
 STA 1097+19.8 TO STA 1102+46.4 RT
 STA 1097+20.1 TO STA 1101+51.8 LT
 STA 1108+71.9 TO STA 1110+42.8 RT
 STA 1108+95.5 TO STA 1112+42.0 LT
 STA 1121+47.5 TO STA 1123+15.5 LT & RT
 STA 1138+20.1 TO STA 1139+92.6 LT & RT



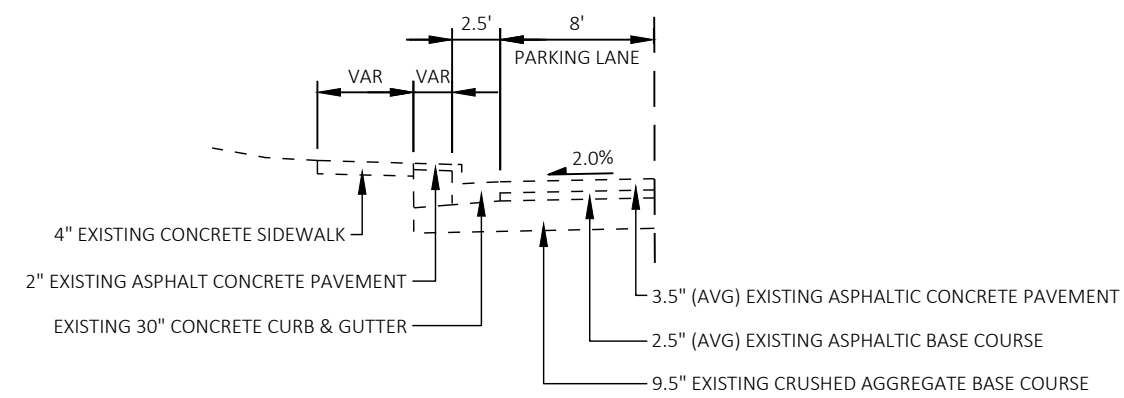
NOTES:

- ① THE CROSS SLOPE ON OUTSIDE 5' OF PAVED SHOULDER VARIES UP TO 4.0%



TYPICAL EXISTING SECTION

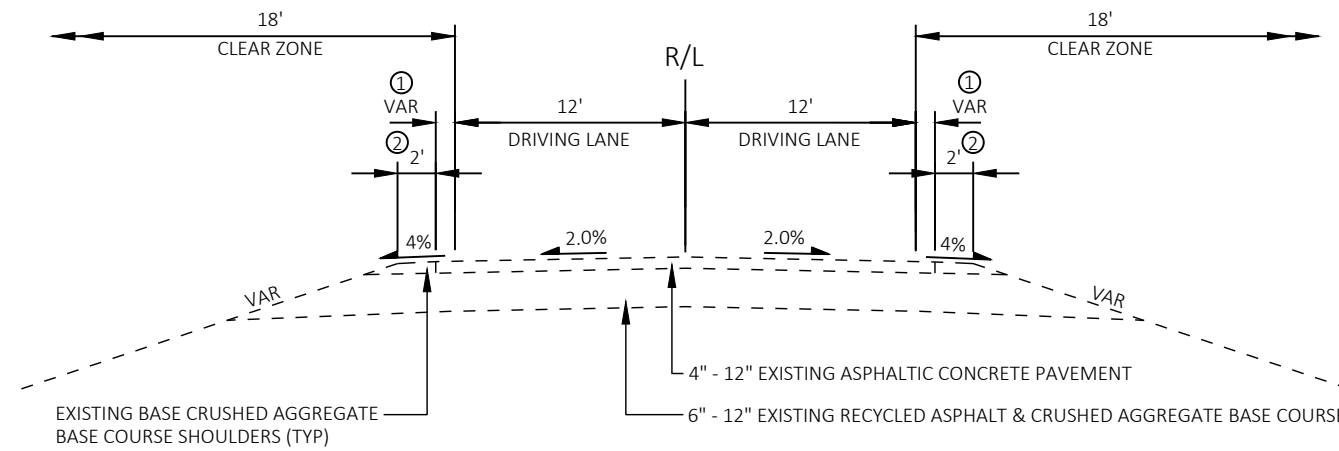
STH 27
STA 1188+54.8 TO STA 1213+62.2



TYPICAL EXISTING SECTION - CURB & GUTTER WITH SIDEWALK

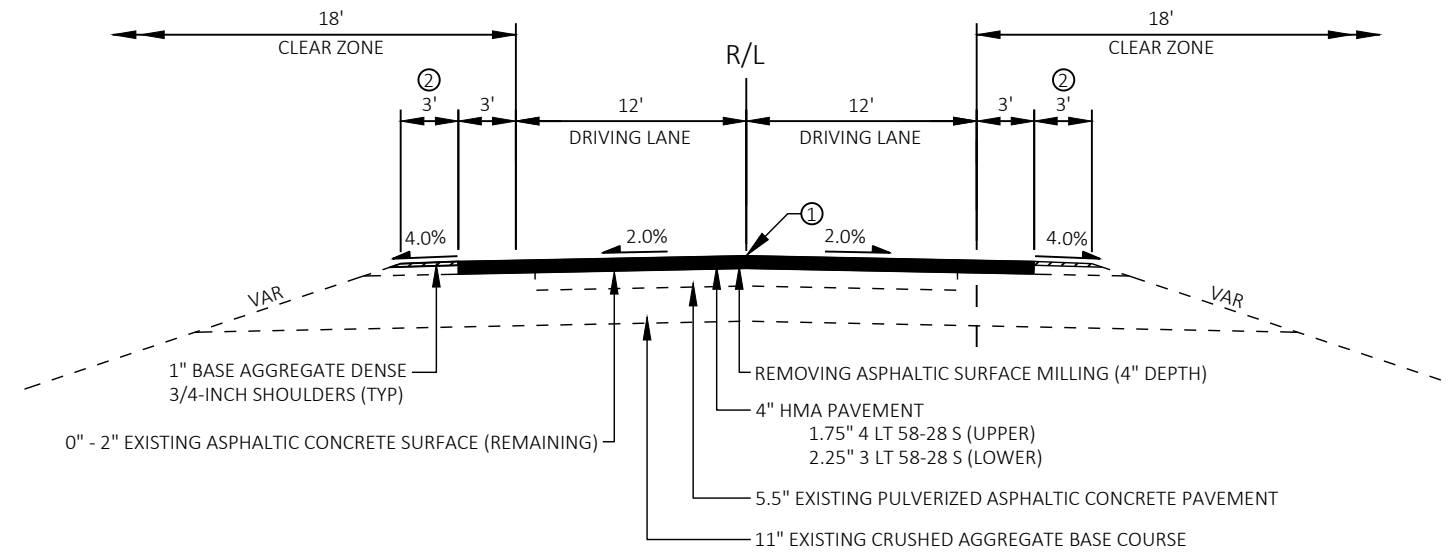
STA 1188+54.8 TO STA 1197+83.0 LT

- NOTES:**
- ① PAVED SHOULDER WIDTH VARIES 0' - 3'.
 - ② GRAVEL SHOULDER WIDTH VARIES 1' - 2'.



TYPICAL EXISTING SECTION

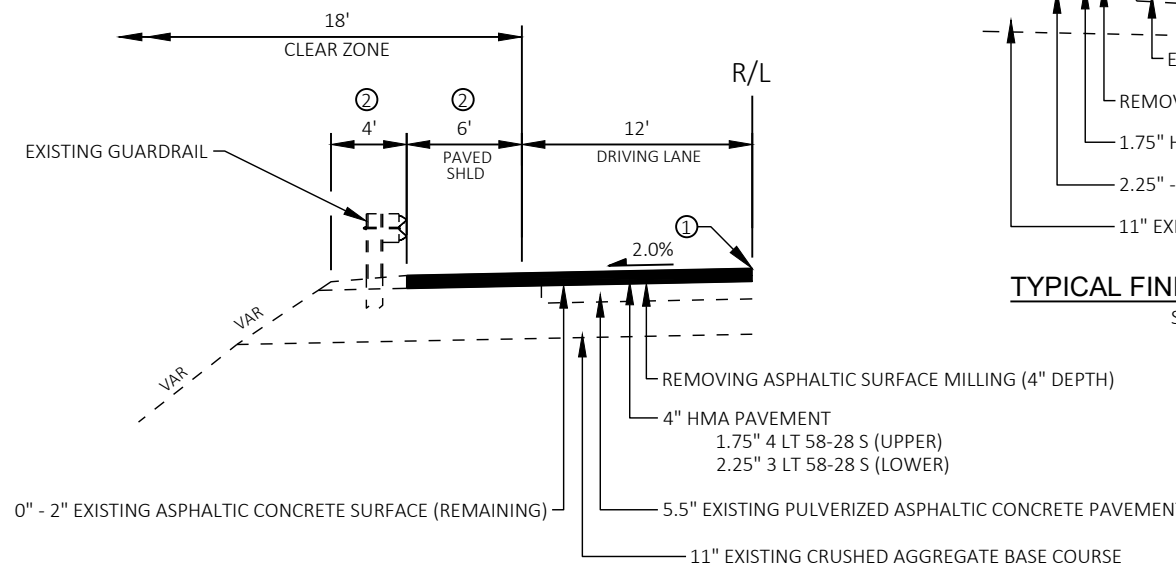
STH 27
STA 1213+62.2 TO STA 1423+49.0



- NOTES:**
- ① CENTERLINE RUMBLE STRIPS:
STA 992+50 TO STA 1169+05
 - ② WIDTH VARIES AT BEAM GUARD EAT LOCATIONS.

TYPICAL FINISHED SECTION

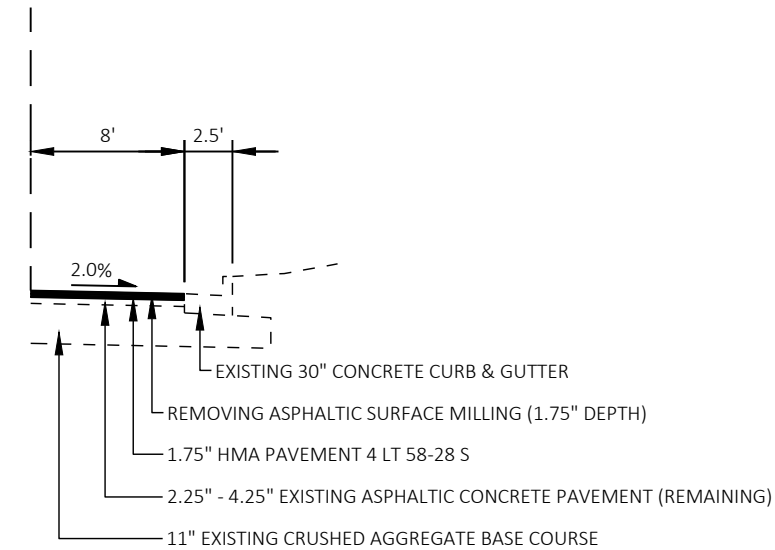
STH 27
STA 979+47.3 TO STA 1175+11.8



TYPICAL FINISHED SECTION - BEAM GUARD

(TYPICAL REPRESENTS BOTH LEFT AND RIGHT SIDES)

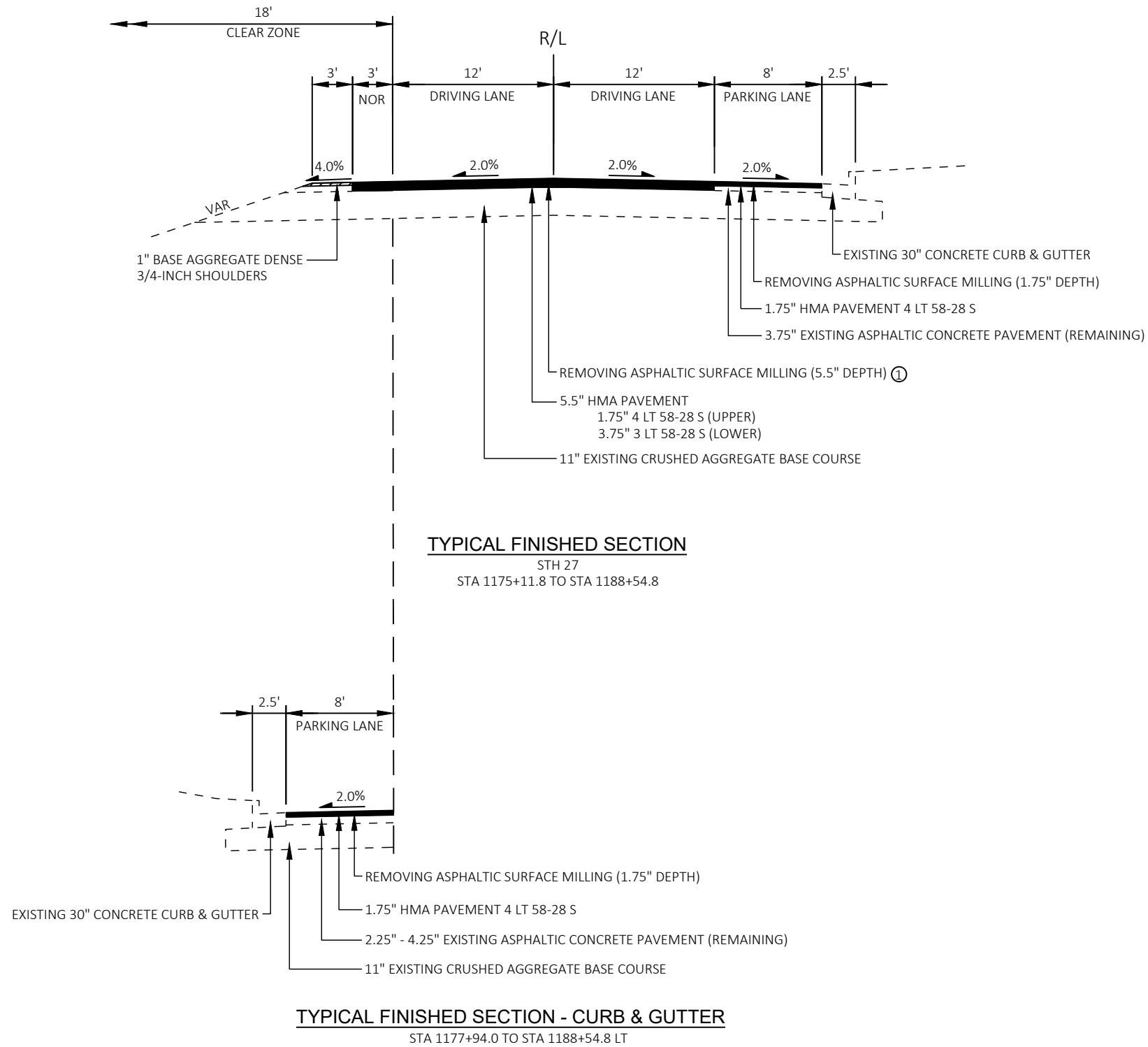
STH 27
STA 991+50.0 TO STA 996+99.7 RT
STA 1003+01.2 TO STA 1008+88.0 RT
STA 1021+15.5 TO STA 1029+18.1 LT
STA 1072+67.9 TO STA 1078+03.5 RT
STA 1097+19.8 TO STA 1102+46.4 RT
STA 1097+20.1 TO STA 1101+51.8 LT
STA 1108+71.9 TO STA 1110+42.8 RT
STA 1108+95.5 TO STA 1112+42.0 LT
STA 1121+47.5 TO STA 1123+15.5 LT & RT
STA 1138+20.1 TO STA 1139+92.6 LT & RT



TYPICAL FINISHED SECTION - CURB & GUTTER

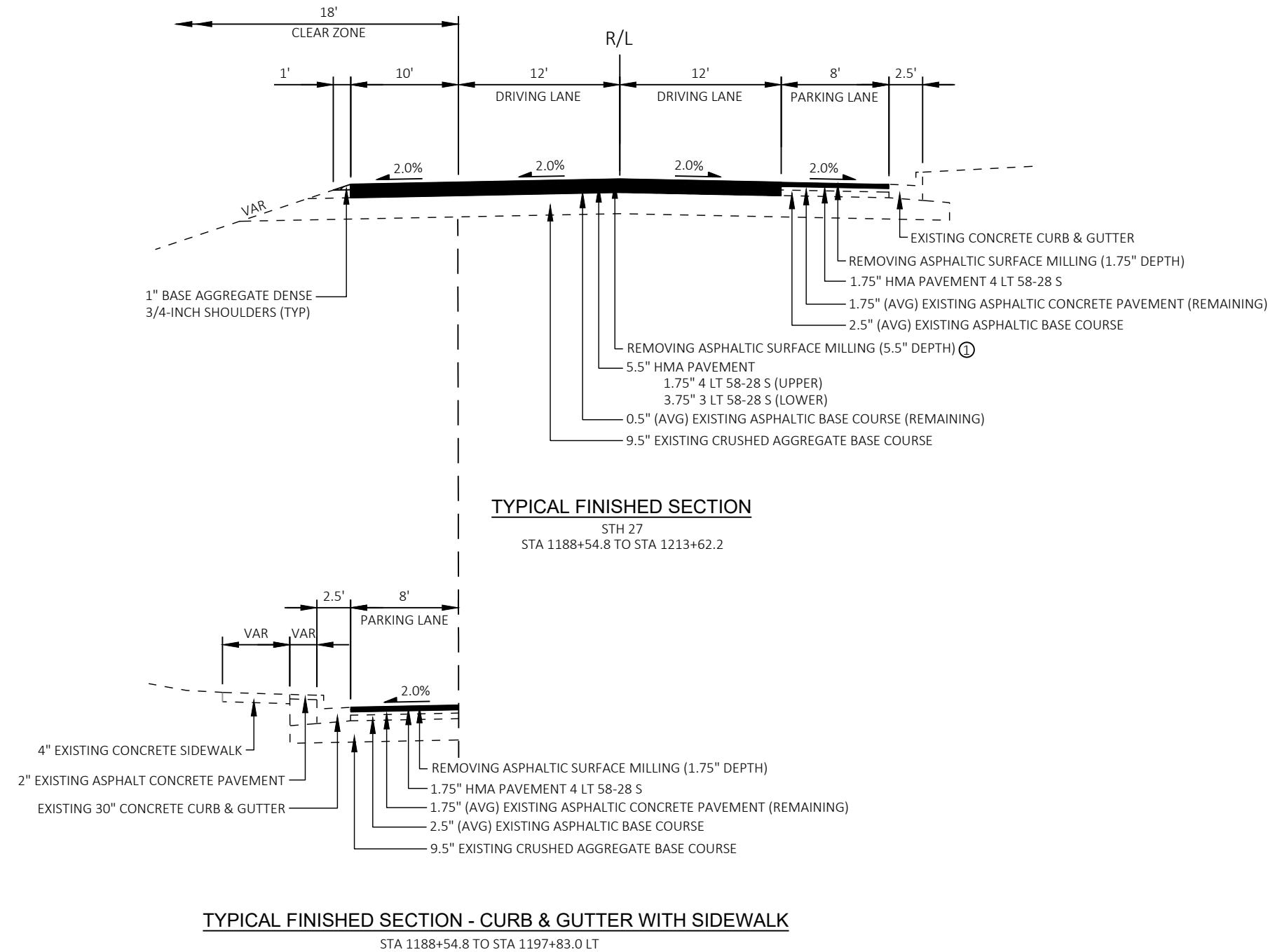
STA 979+47.3 TO STA 981+28.3 RT

- NOTES:**
- ① EXTEND MILLING DEPTH INTO EXISTING AGGREGATE BASE COURSE AS NEEDED TO REMOVE 5.5" OF TOTAL MATERIAL.

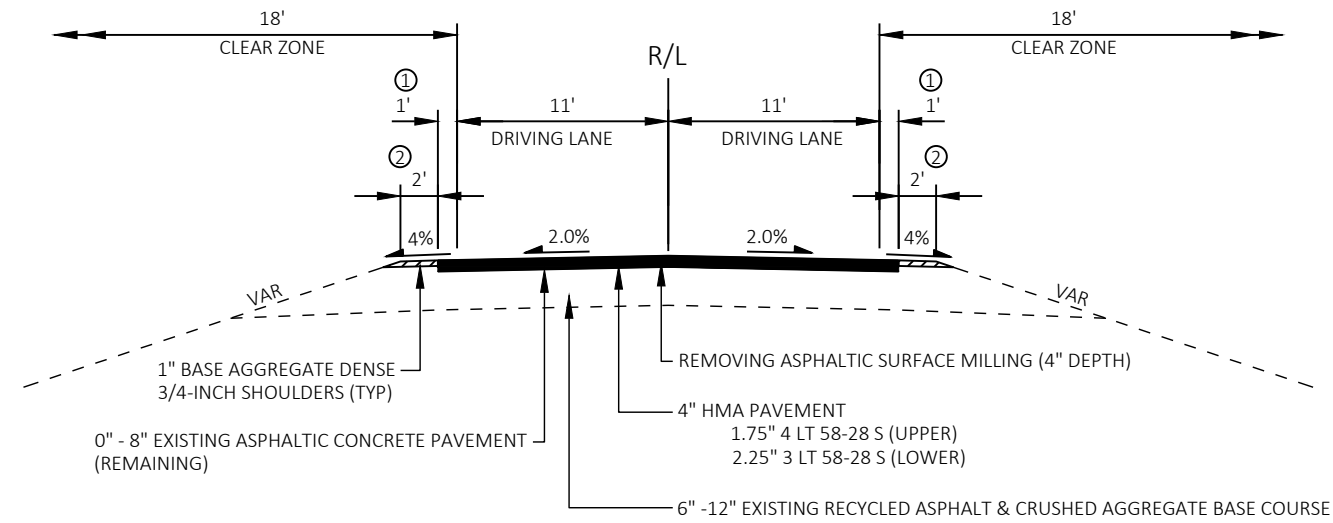


NOTES:

- ① EXTEND MILLING DEPTH INTO EXISTING AGGREGATE BASE COURSE AS NEEDED TO REMOVE 5.5" OF TOTAL MATERIAL.

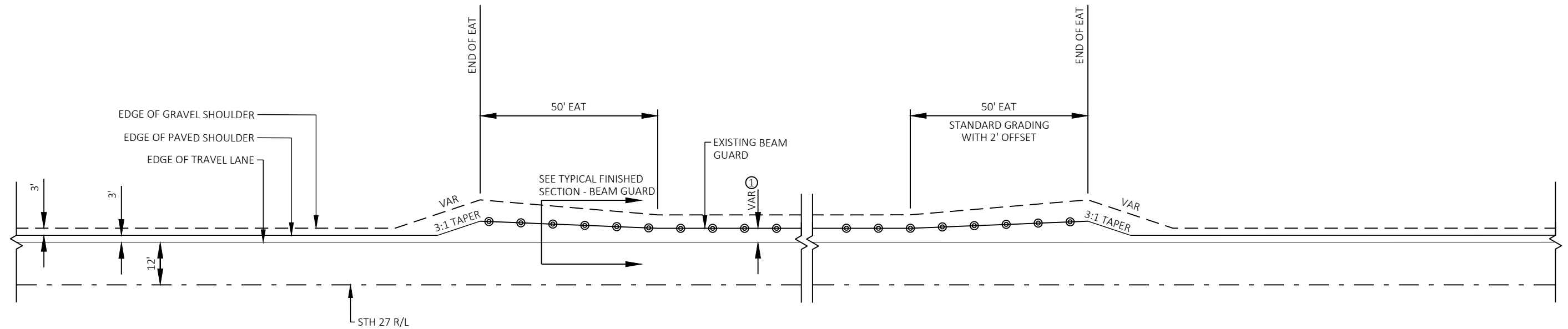


- NOTES:**
- ① PAVED SHOULDER WIDTH VARIES 0' - 3'.
 - ② GRAVEL SHOULDER WIDTH VARIES 1' - 2'.



TYPICAL FINISHED SECTION

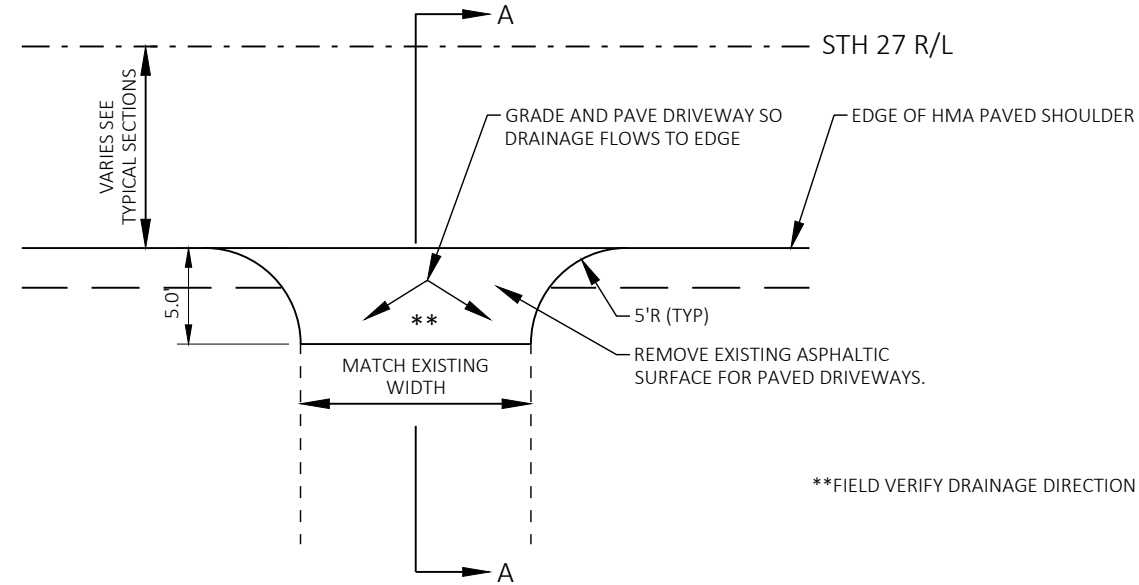
STH 27
STA 1213+62.2 TO STA 1423+49.0



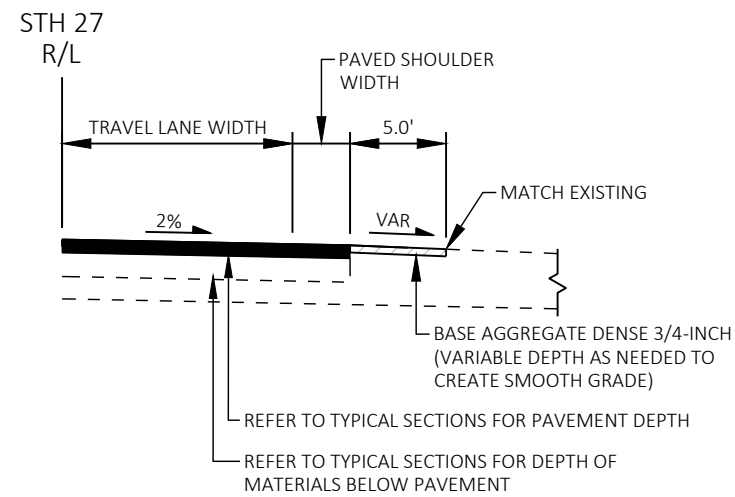
ASPHALTIC SHOULDER AT BEAM GUARD

NOTES:

- ① PAVE TO FACE OF RAIL INCLUDING ALONG THE EAT.



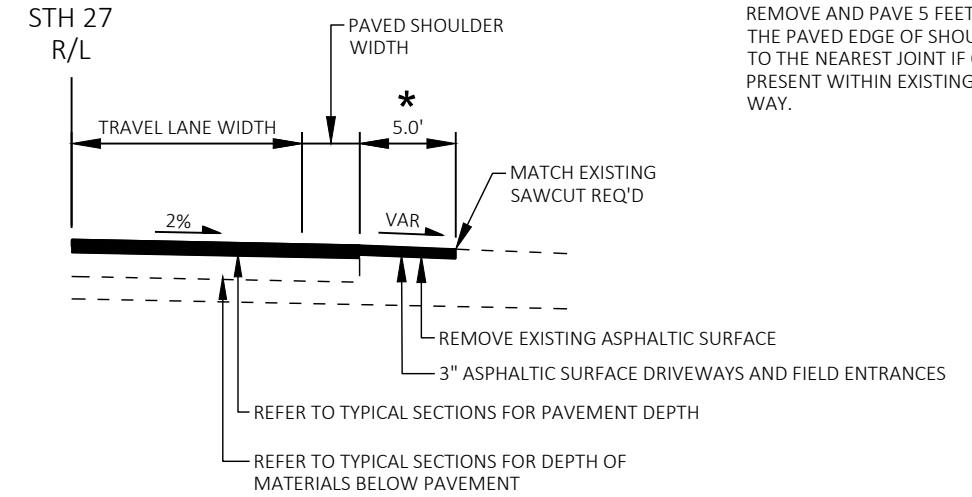
DRIVEWAY/INTERSECTION



SECTION A-A DRIVEWAY PAVING - GRAVEL DRIVEWAY/INTERSECTION

STA 979+70, 24' LT	STA 1078+07, 22' RT	STA 1155+48, 20' LT (F.E.)	BROCKWAY LANE	STA 1397+30, 17' RT
STA 981+80, 25' RT	STA 1084+03, 20' LT	STA 1160+44, 20' RT	STA 1284+17, 17' LT	STA 1398+09, 17' RT
STA 985+05, 20' LT (F.E.)	STA 1090+66, 20' RT	STA 1166+61, 20' RT (C.E.)	STA 1285+47, 17' RT	STA 1399+17, 26' RT
STA 990+54, 20' LT	STA 1091+41, 20' LT (F.E.)	STA 1173+38, 20' RT (F.E.)	STA 1298+94, 17' LT	STA 1400+38, 26' RT
STA 994+45, 20' LT	STA 1096+14, 20' RT (F.E.)	STA 1202+32, 25' LT	STA 1302+49, 17' RT	STA 1400+40, 25' LT
STA 1002+88, 23' RT (F.E.)	STA 1108+52, 25' RT	STA 1207+71, 25' LT (F.E.)	STA 1315+36, 17' RT	STA 1400+94, 25' RT
STA 1016+90, 22' RT	STA 1124+01, 20' LT (F.E.)	STA 1214+54, 20' RT	STA 1325+25, 17' LT	STA 1402+35, 19' RT
STA 1017+08, 20' LT (F.E.)	STA 1125+23, 20' RT	STA 1219+52, 17' RT	STA 1329+90, 18' RT	STA 1403+50, 17' RT
INDIGO ROAD	STA 1137+62, 20' RT	MOORE DRIVE	STA 1334+02, 23' RT	STA 1404+46, 17' LT (C.E.)
STA 1031+87, 20' RT	STA 1137+65, 20' LT	STA 1231+02, 17' RT (F.E.)	STA 1339+26, 17' RT (C.E.)	STA 1405+44, 17' LT
STA 1034+36, 20' LT	STA 1148+94, 20' LT (F.E.)	STA 1231+13, 17' LT	COYOTE DRIVE	STA 1405+47, 17' RT (F.E.)
STA 1042+35, 20' RT (F.E.)	STA 1149+11, 20' RT (F.E.)	STA 1243+33, 17' LT (F.E.)	STA 1359+69, 17' RT	STA 1420+66, 17' RT
STA 1043+23, 20' LT (F.E.)	STA 1153+84, 20' LT	STA 1243+43, 17' RT	STA 1365+99, 17' RT	STA 1421+24, 17' LT (F.E.)
STA 1060+51, 20' RT (F.E.)	STA 1155+42, 20' RT	STA 1261+70, 17' RT	STA 1396+68, 17' LT	STA 1421+43, 17' RT (C.E.)
				STA 1422+37, 17' RT

(ALL P.E.'S UNLESS OTHERWISE NOTED)



SECTION A-A DRIVEWAY PAVING - ASPHALTIC DRIVEWAY

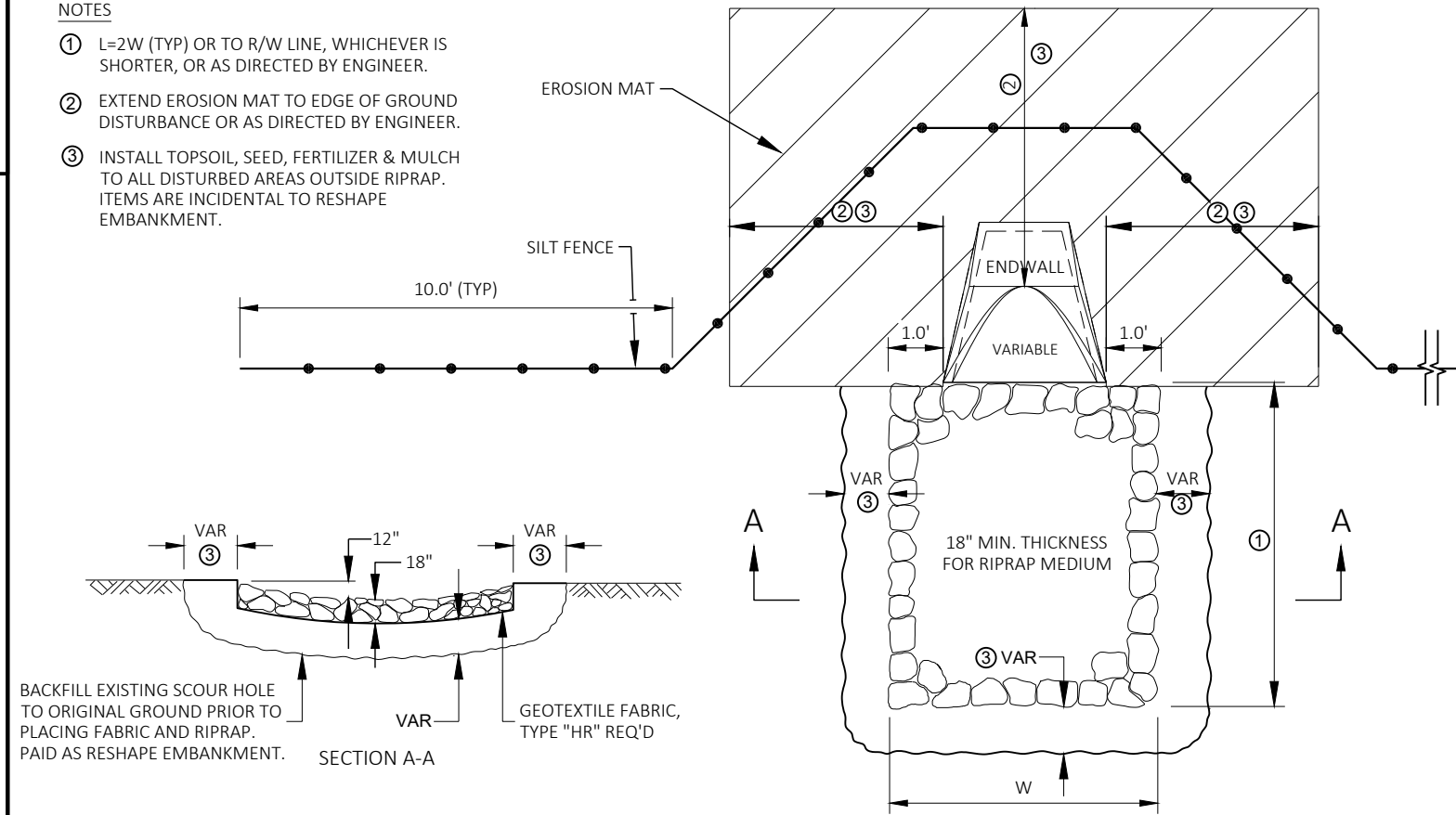
STA 983+50, 29' RT
STA 1065+29, 29' RT
STA 1072+64, 33' LT
STA 1077+62, 20' LT (C.E.)
STA 1101+81, 83' LT
STA 1166+66, 20' LT
STA 1171+17, 20' LT
STA 1173+30, 20' LT (C.E.)
STA 1174+29, 20' LT (C.E.)
STA 1175+52, 37' LT
STA 1205+78, 25' LT
STA 1213+34, 25' LT
STA 1248+81, 17' LT
STA 1250+10, 17' LT
STA 1330+29, 19' LT
STA 1332+10, 24' LT
STA 1332+16, 29' RT
STA 1378+24, 17' RT
STA 1391+18, 17' RT
STA 1422+63, 26' LT

(ALL P.E.'S UNLESS OTHERWISE NOTED)

*NOTE: REMOVE AND PAVE 5 FEET BEYOND THE PAVED EDGE OF SHOULDER OR TO THE NEAREST JOINT IF ONE IS PRESENT WITHIN EXISTING RIGHT OF WAY.

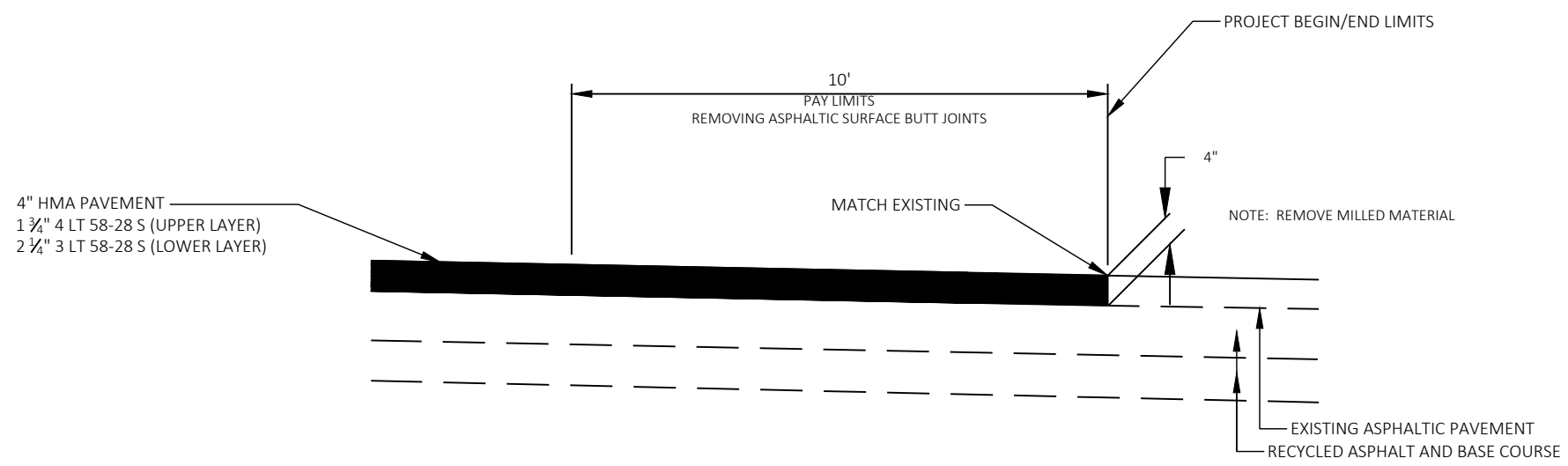
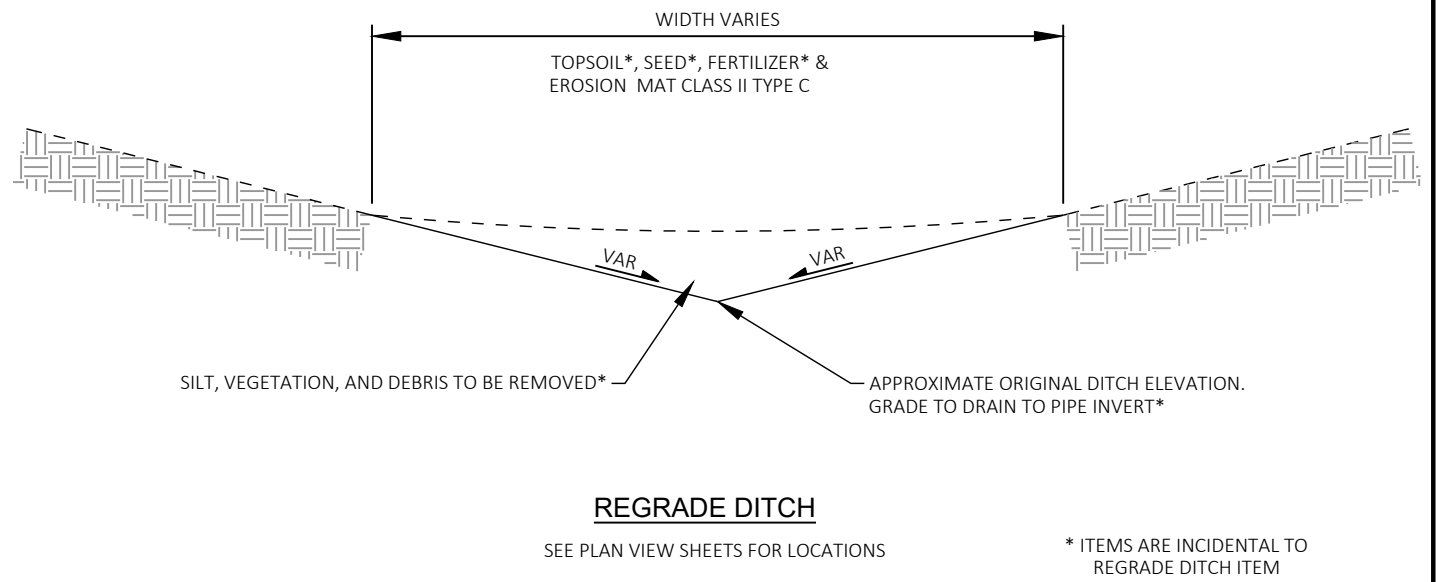
NOTES

- ① L=2W (TYP) OR TO R/W LINE, WHICHEVER IS SHORTER, OR AS DIRECTED BY ENGINEER.
- ② EXTEND EROSION MAT TO EDGE OF GROUND DISTURBANCE OR AS DIRECTED BY ENGINEER.
- ③ INSTALL TOPSOIL, SEED, FERTILIZER & MULCH TO ALL DISTURBED AREAS OUTSIDE RIPRAP. ITEMS ARE INCIDENTAL TO RESHAPE EMBANKMENT.

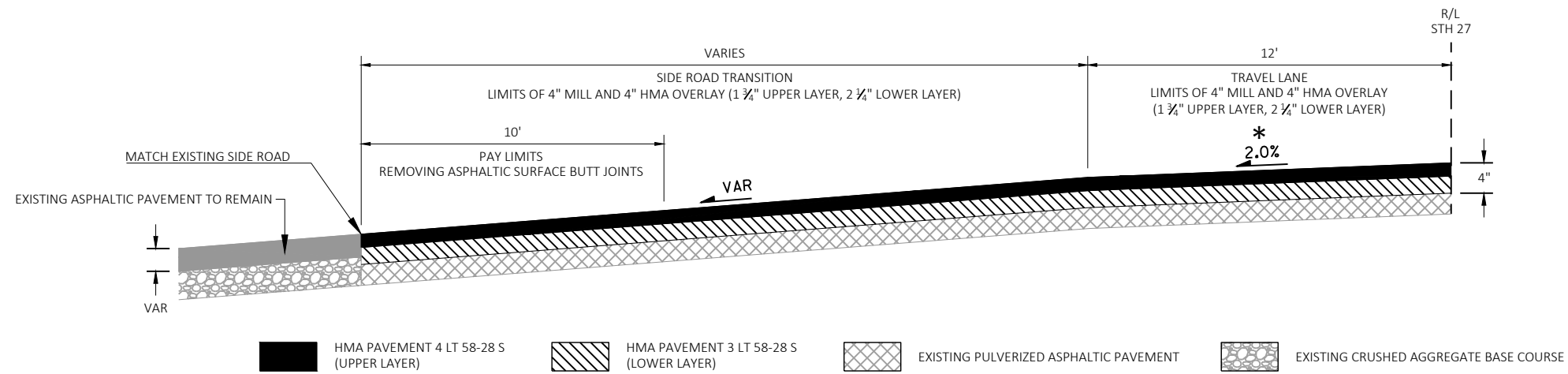
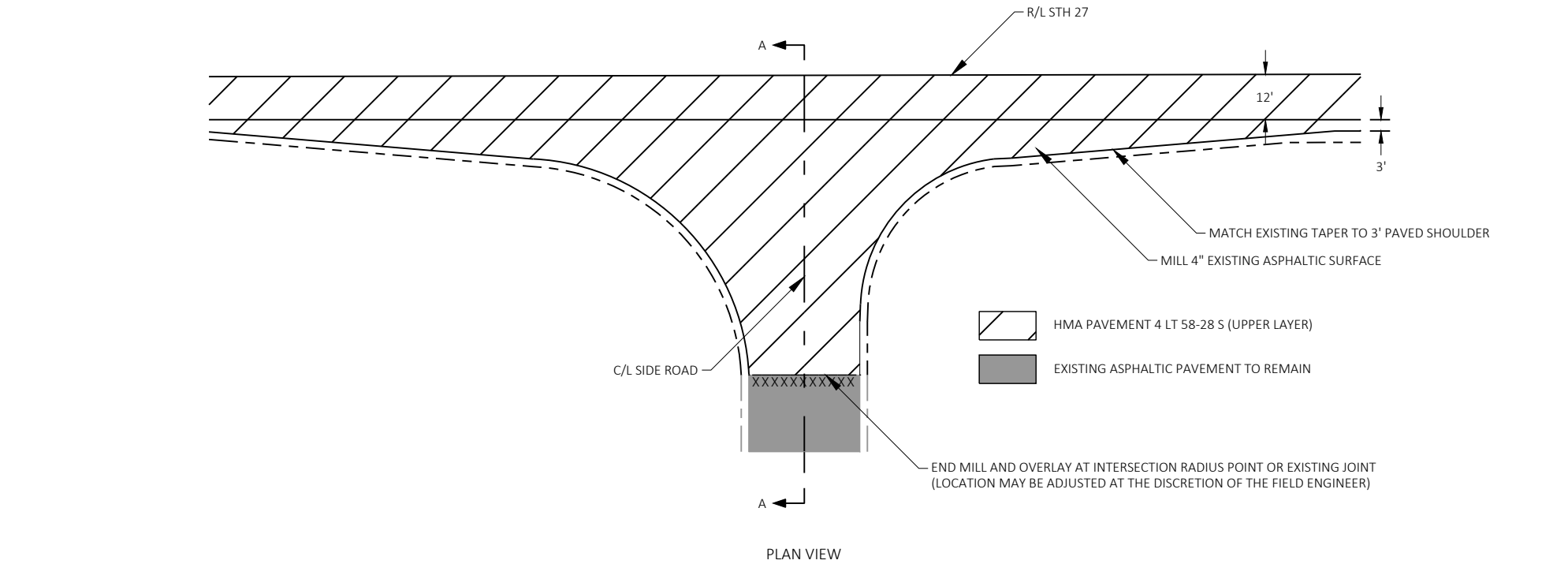


RIPRAP, EROSION MAT AND SILT FENCE TREATMENT AT CULVERT ENDWALLS

SEE PLAN VIEW SHEETS FOR LOCATIONS



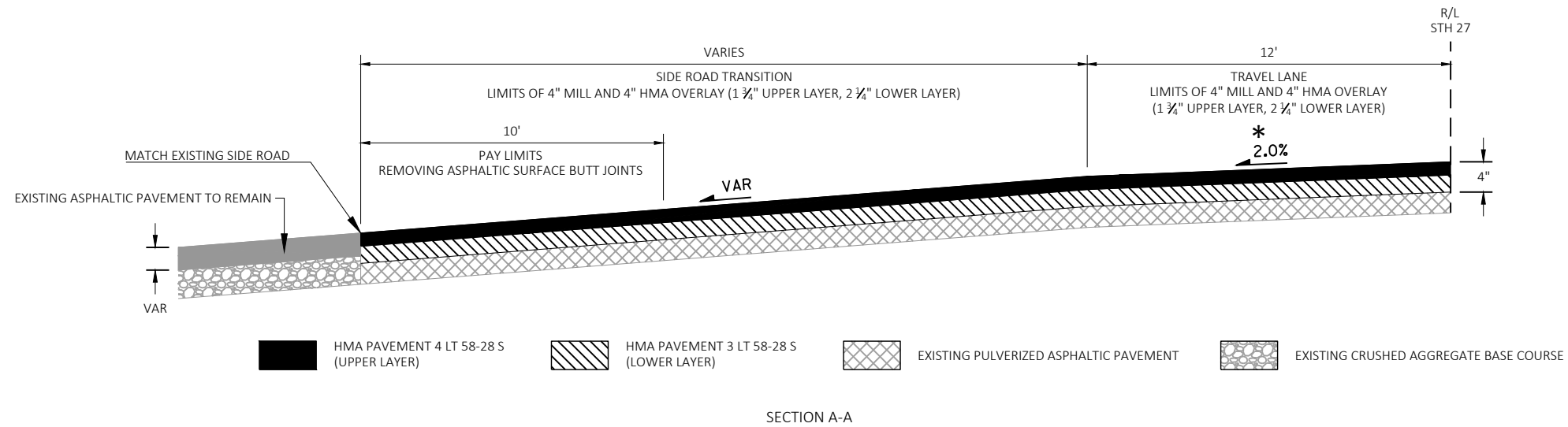
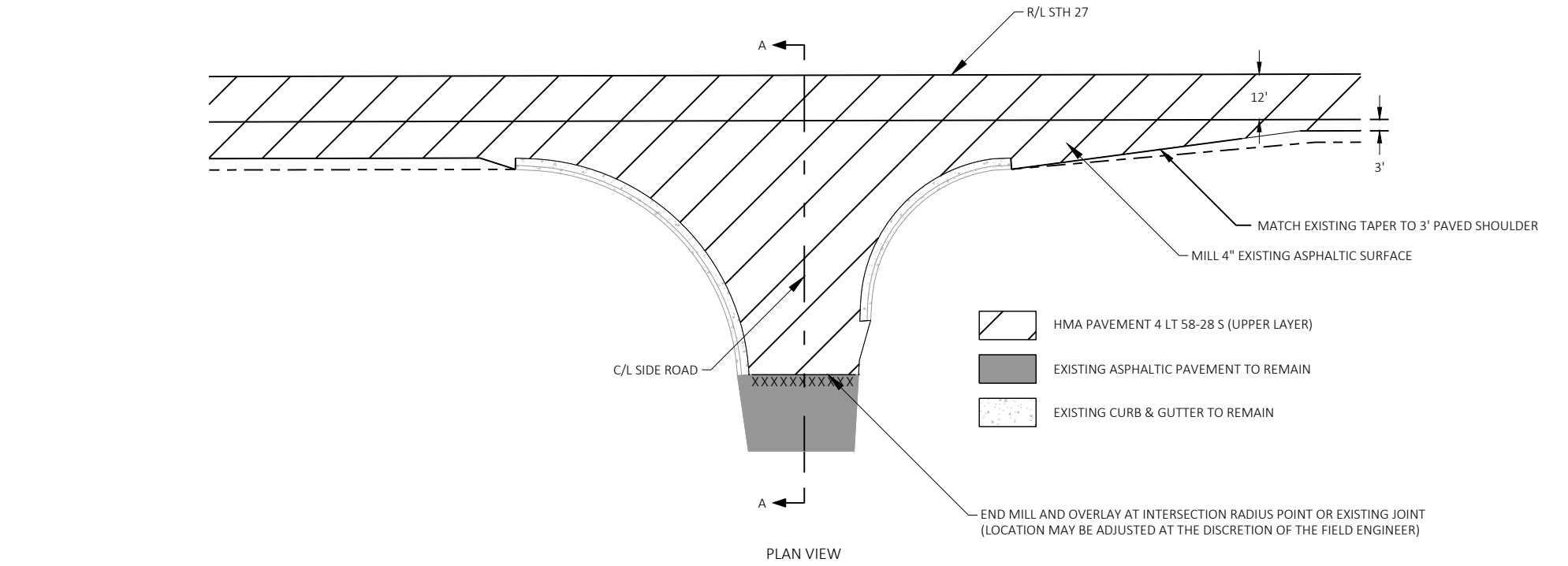
STH 27 MAINLINE BUTT JOINTS



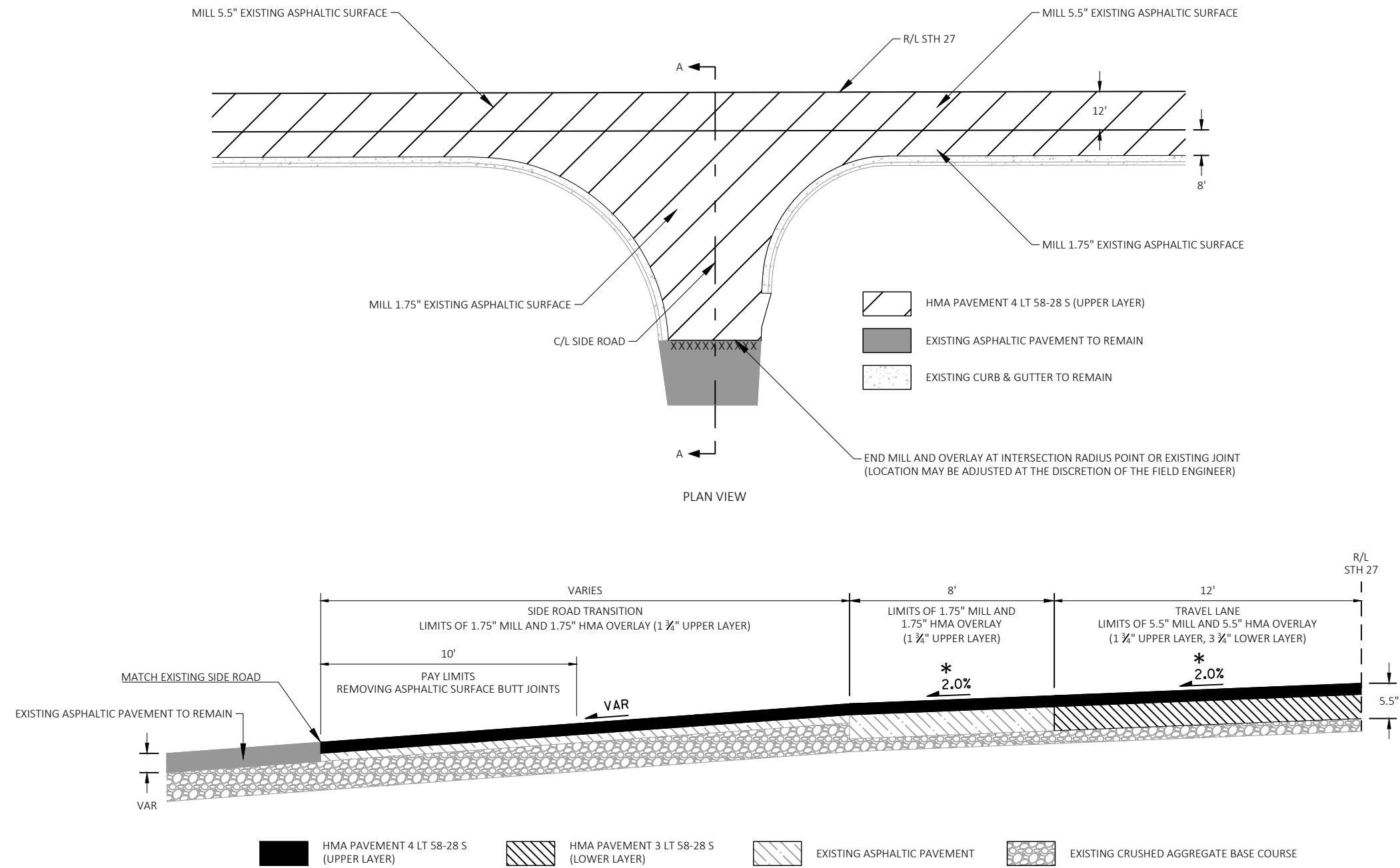
SIDE ROAD TRANSITION DETAIL

*2.0% TYPICAL; MATCH EXISTING PAVEMENT CROSS SLOPE ALONG SUPERELEVATED CURVES

- SENECA HEIGHTS AVENUE
- SNYDER ROAD
- COUNTRY VIEW DRIVE
- STONE LANE
- STONE POINT ROAD
- BIRD ROAD
- OLSEN FEED ROAD
- ASPENSON ROAD
- ROUNDS ROAD



SIDE ROAD WITH CURB AND GUTTER TRANSITION DETAIL
 *2.0% TYPICAL; MATCH EXISTING PAVEMENT CROSS SLOPE ALONG SUPERELEVATED CURVES
 WEST STREET (SOUTH)
 CTH S



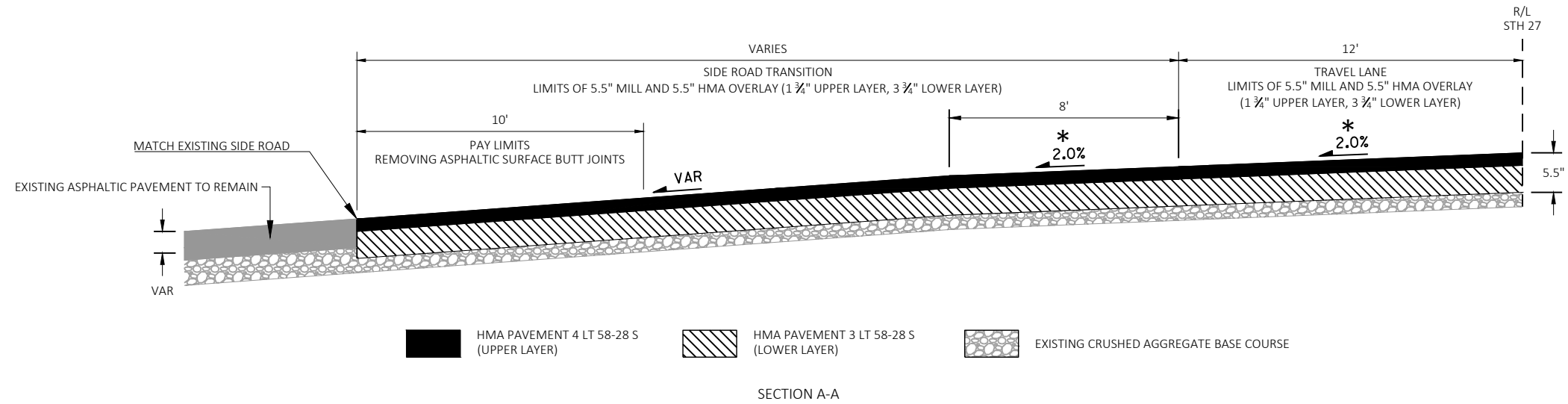
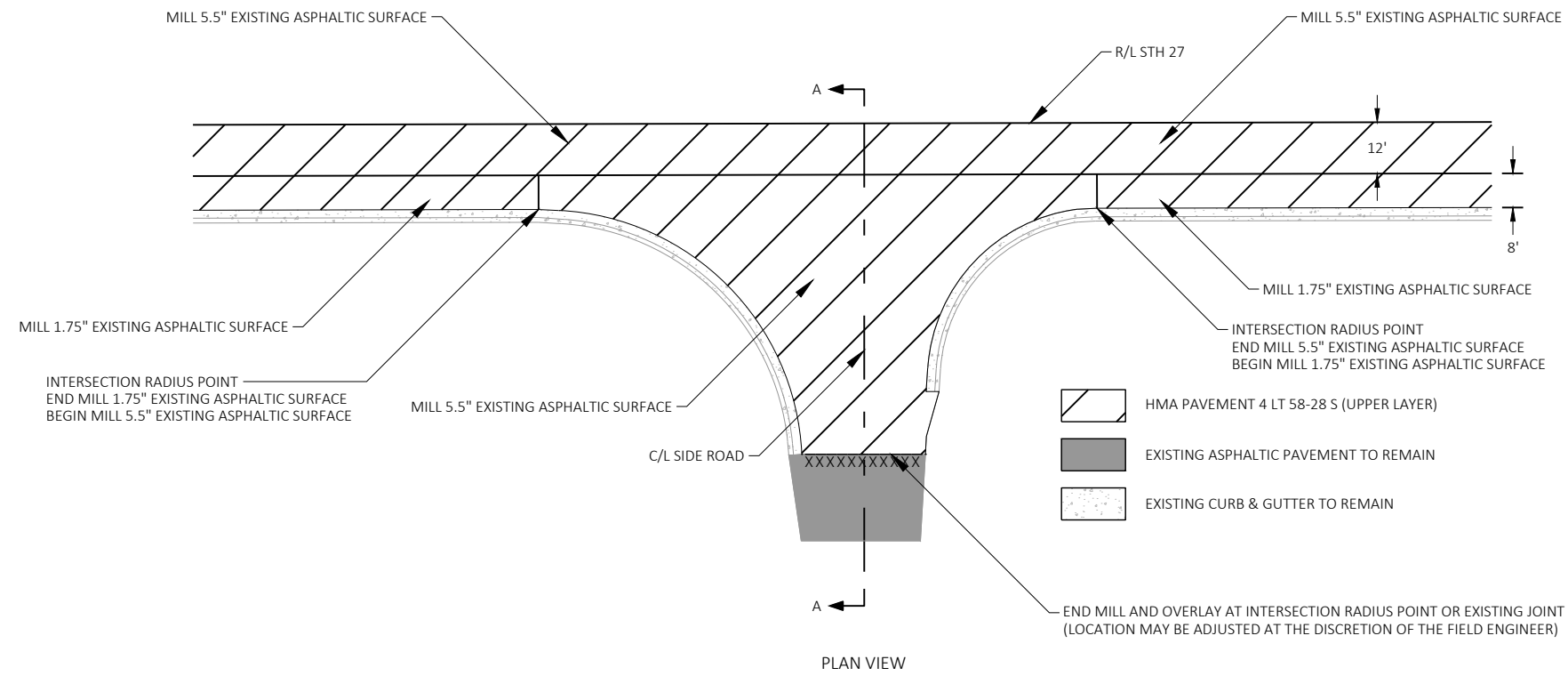
SECTION A-A

URBAN SIDE ROAD WITH CURB AND GUTTER TRANSITION DETAIL

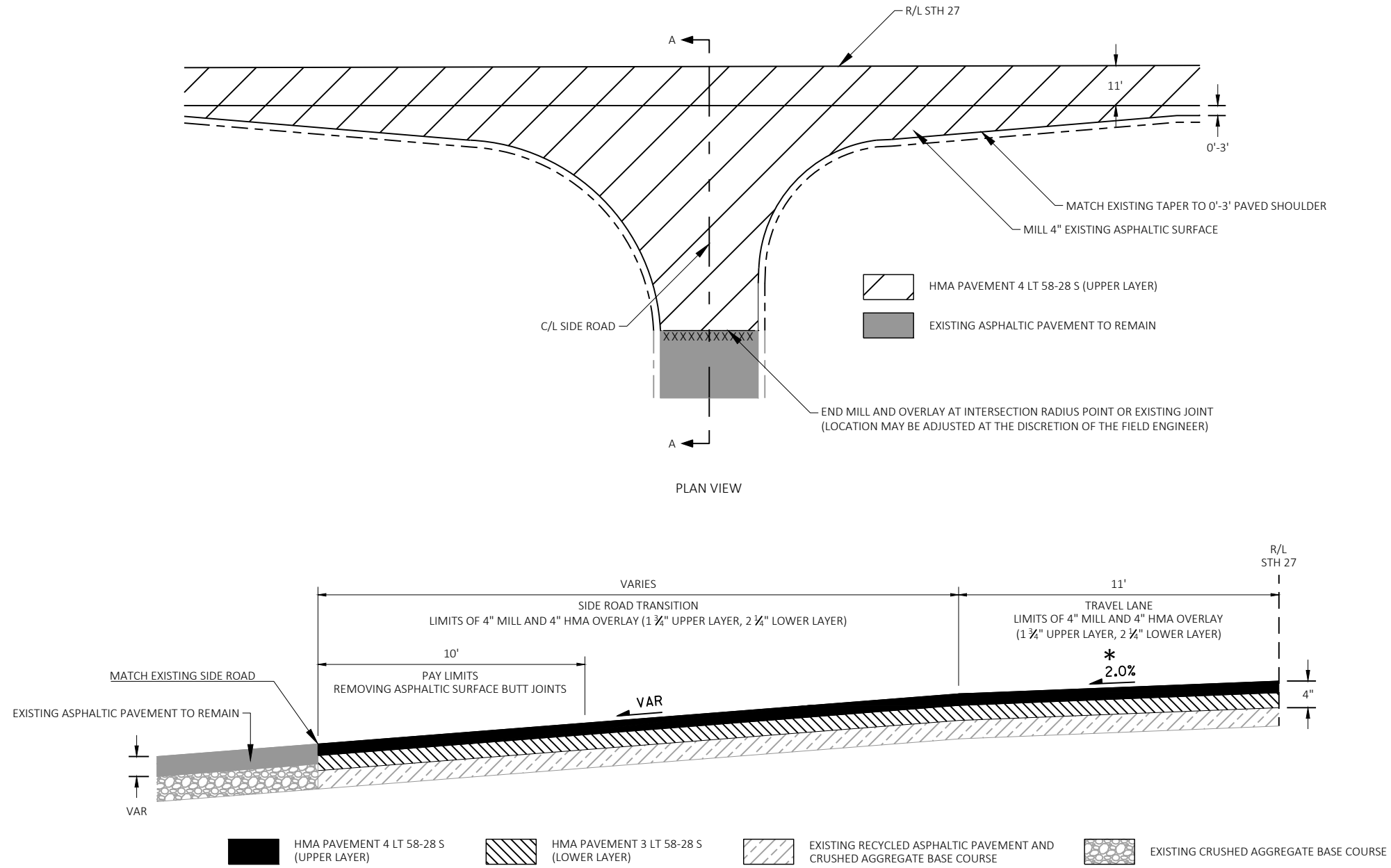
*2.0% TYPICAL; MATCH EXISTING PAVEMENT CROSS SLOPE ALONG SUPERELEVATED CURVES

- 1ST STREET
- 3RD STREET
- 4TH STREET
- WEST STREET (NORTH)
- 5TH STREET
- PRIVATE ENTRANCE**
- HELGERSON STREET

**DO NOT MILL AND PAVE AROUND THE ENTIRE CURB RADIUS. PROVIDE BUTT JOINT 25' LT OF STH 27 R/L FOR MATCH TO EXISTING LT PAVED SHOULDER ON STH 27 SB FOR MILLING AND PAVING LIMITS. NO BUTT JOINT REQUIRED.



URBAN SIDE ROAD WITH CURB AND GUTTER TRANSITION DETAIL
 *2.0% TYPICAL; MATCH EXISTING PAVEMENT CROSS SLOPE ALONG SUPERELEVATED CURVES
 STH 171/2ND STREET
 STH 171/DIAGONAL STREET
 2ND STREET



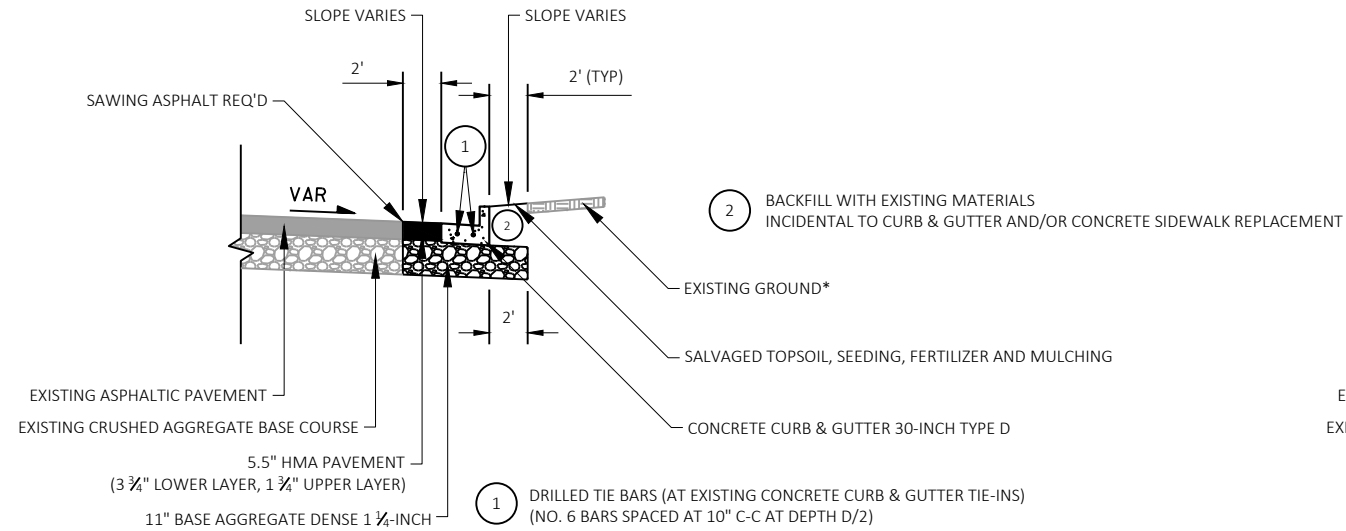
PLAN VIEW

SECTION A-A

SIDE ROAD TRANSITION DETAIL

*2.0% TYPICAL; MATCH EXISTING PAVEMENT CROSS SLOPE ALONG SUPERELEVATED CURVES

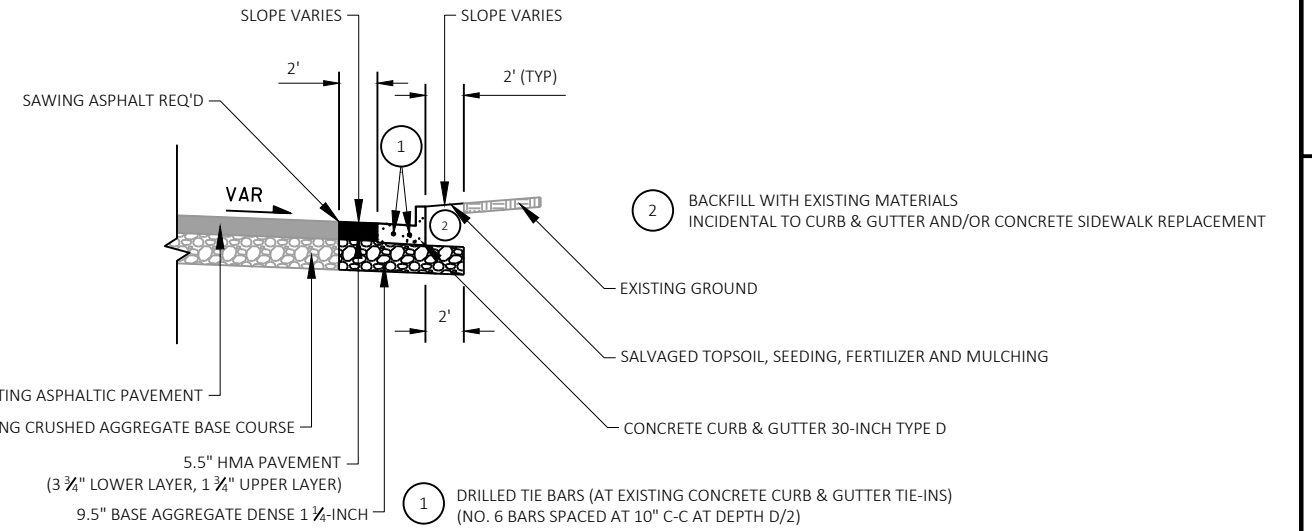
- ROLLER COASTER ROAD
- HELLAND ROAD
- FRYSETH DRIVE
- BOB WHITE GLEN ROAD
- BOMA RIDGE ROAD
- JOHNSTOWN ROAD



*NOTE: EXISTING 4" SIDEWALK FROM STA 1186+40 TO STA 1186+56 LT

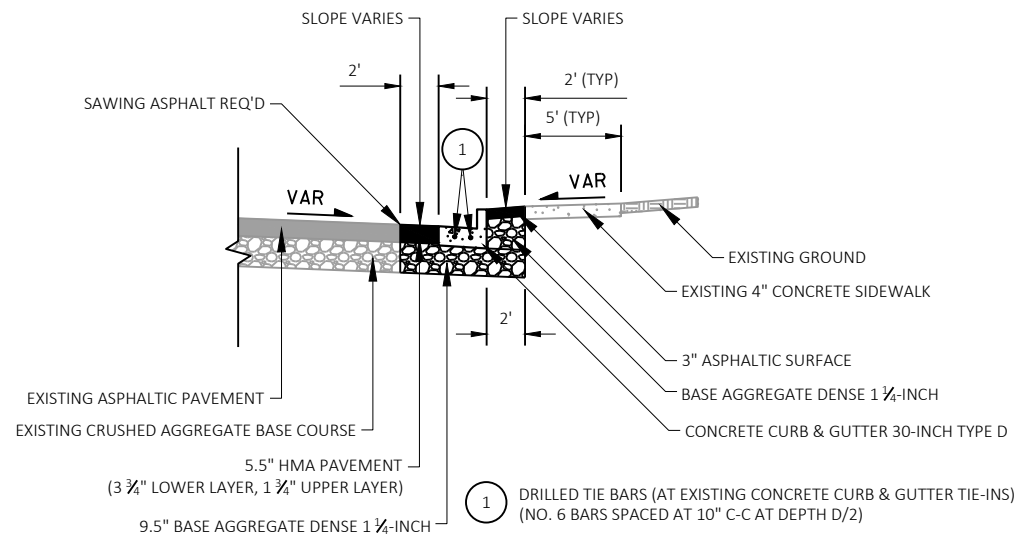
CURB & GUTTER REPLACEMENT WITH GRASSED TERRACE DETAIL (SOUTH OF STH 171)

- | | |
|------------------------------|------------------------------|
| STA 1177+94 - STA 1178+10 LT | STA 1175+77 - STA 1176+00 RT |
| STA 1178+53 - STA 1178+76 LT | STA 1176+38 - STA 1176+52 RT |
| STA 1179+04 - STA 1179+19 LT | STA 1176+97 - STA 1177+12 RT |
| STA 1179+79 - STA 1180+06 LT | STA 1177+27 - STA 1177+42 RT |
| STA 1186+40 - STA 1186+56 LT | STA 1185+14 - STA 1185+28 RT |
| | STA 1185+65 - STA 1185+75 RT |



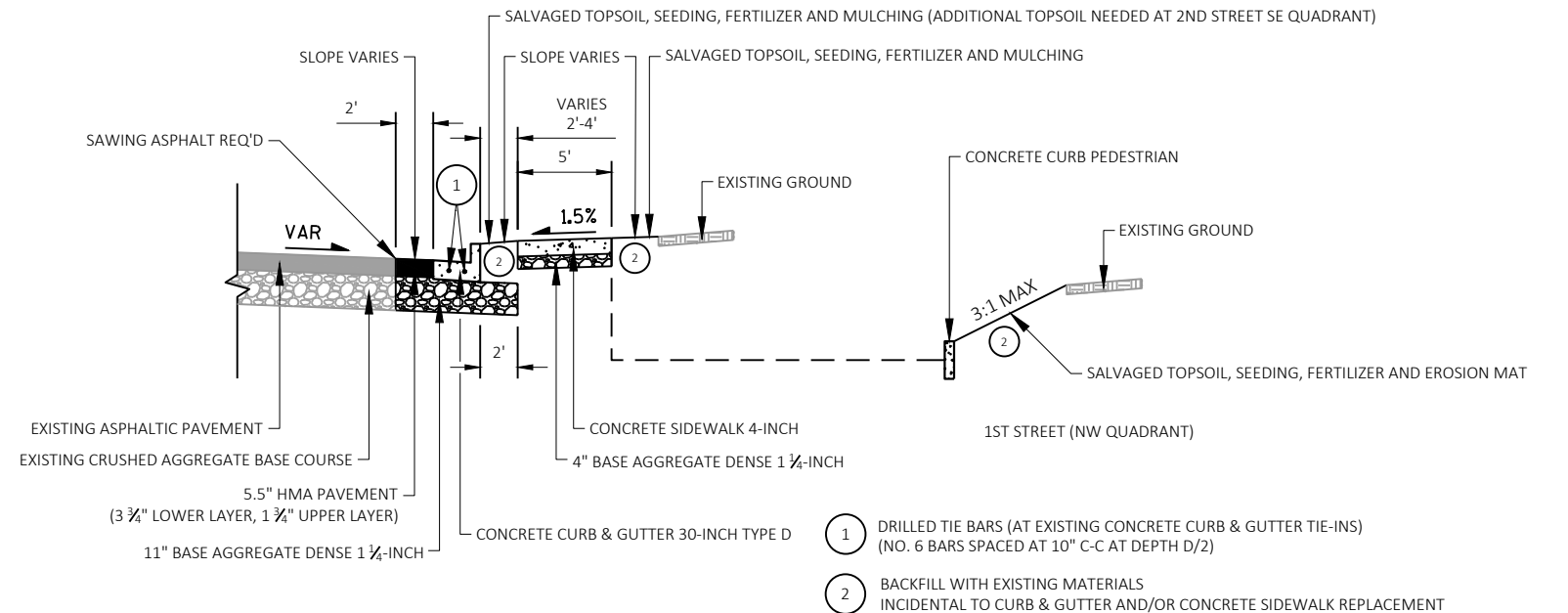
CURB & GUTTER REPLACEMENT WITH GRASSED TERRACE DETAIL (NORTH OF STH 171)

- | |
|------------------------------|
| STA 1191+33 - STA 1191+44 RT |
| STA 1191+91 - STA 1192+08 RT |
| STA 1198+35 - STA 1198+53 RT |
| STA 1211+86 - STA 1211+87 RT |
| STA 1211+93 - STA 1212+16 RT |
| STA 1212+81 - STA 1213+62 RT |



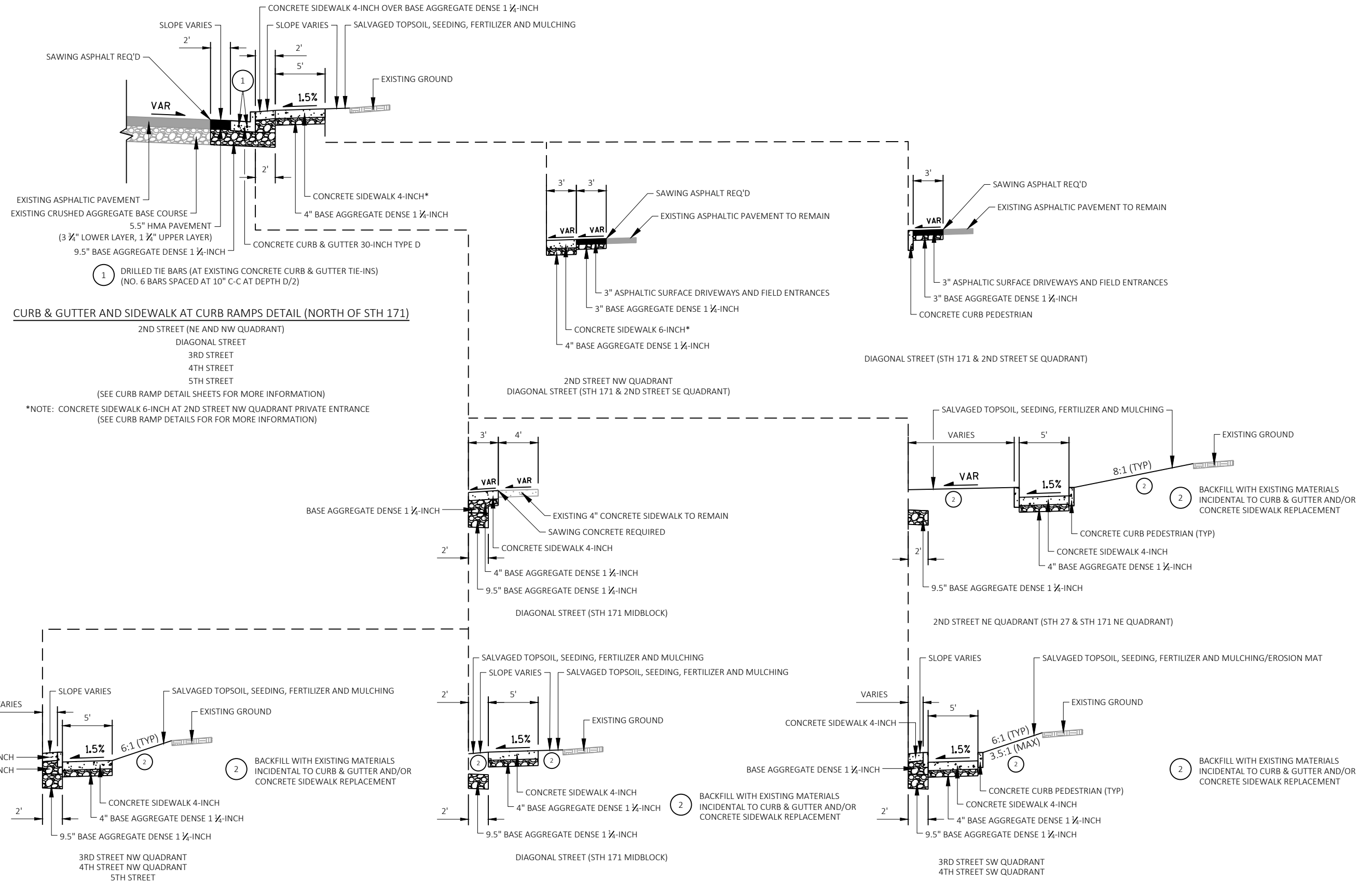
CURB & GUTTER REPLACEMENT WITH ASPHALT TERRACE DETAIL (NORTH OF STH 171)

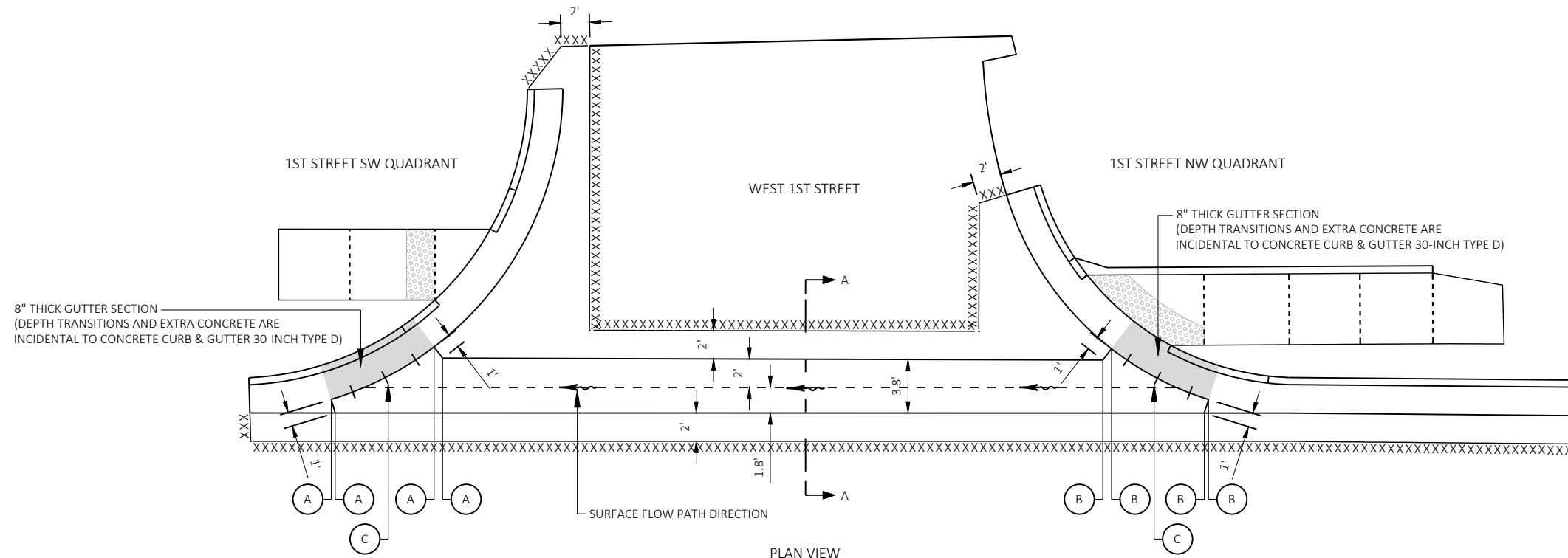
- | |
|------------------------------|
| STA 1190+92 - STA 1191+09 LT |
| STA 1196+69 - STA 1196+87 LT |



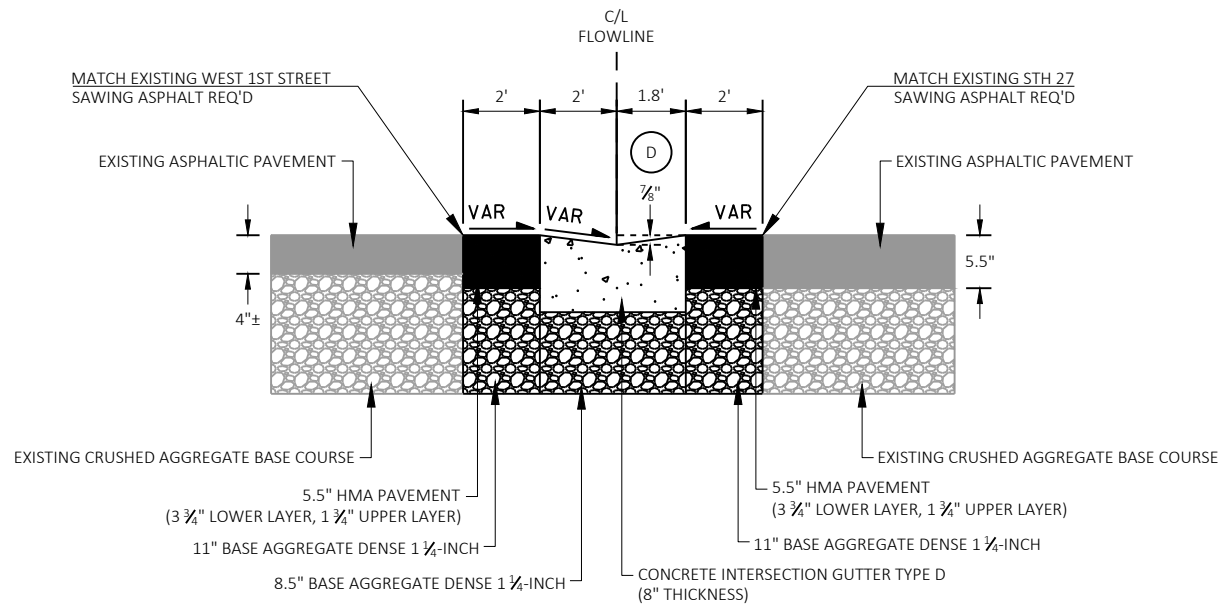
CURB & GUTTER AND SIDEWALK AT CURB RAMPS DETAIL (SOUTH OF STH 171)

- 1ST STREET
2ND STREET (SE AND SW QUADRANTS)
(SEE CURB RAMP DETAIL SHEETS FOR MORE INFORMATION)





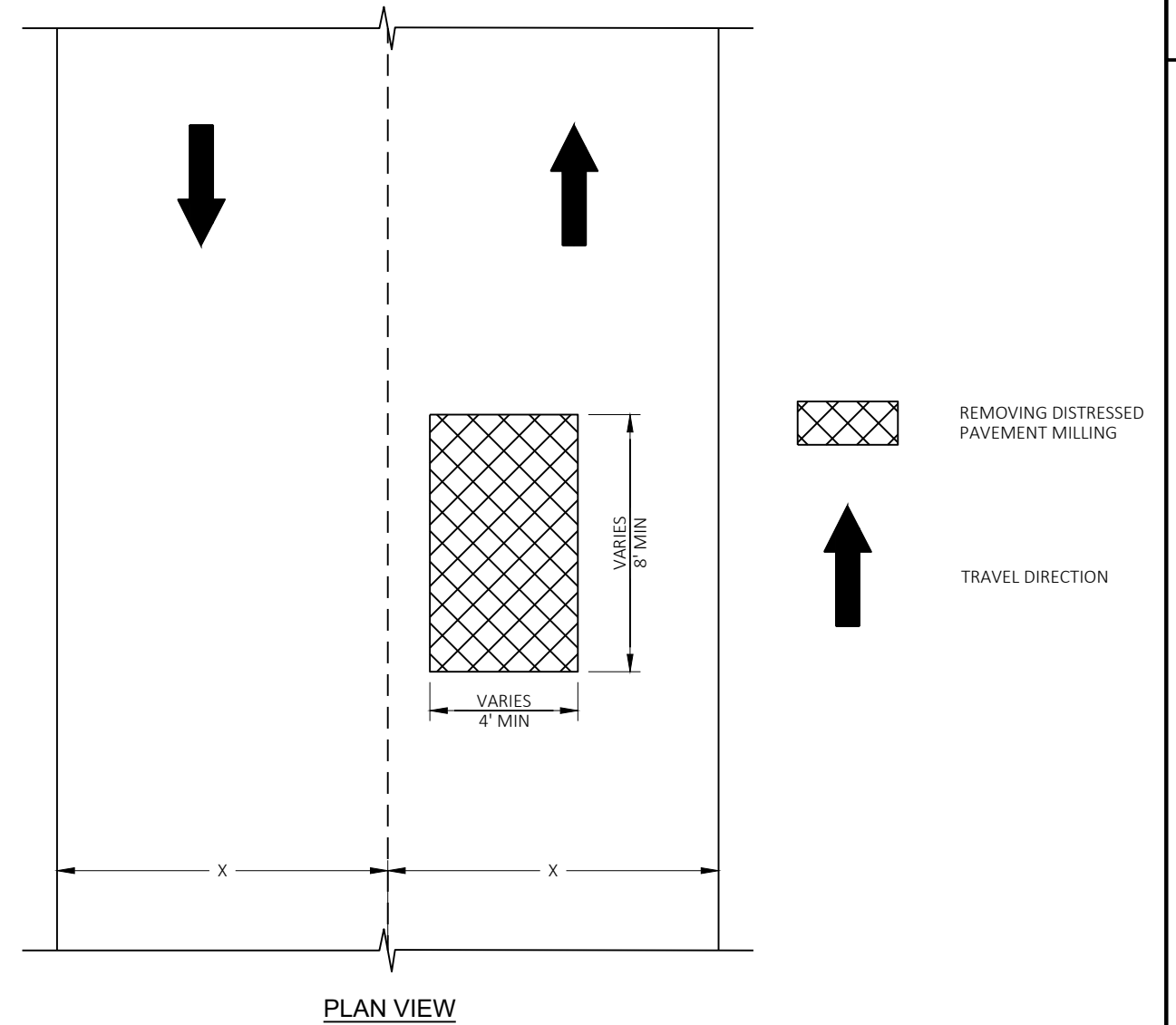
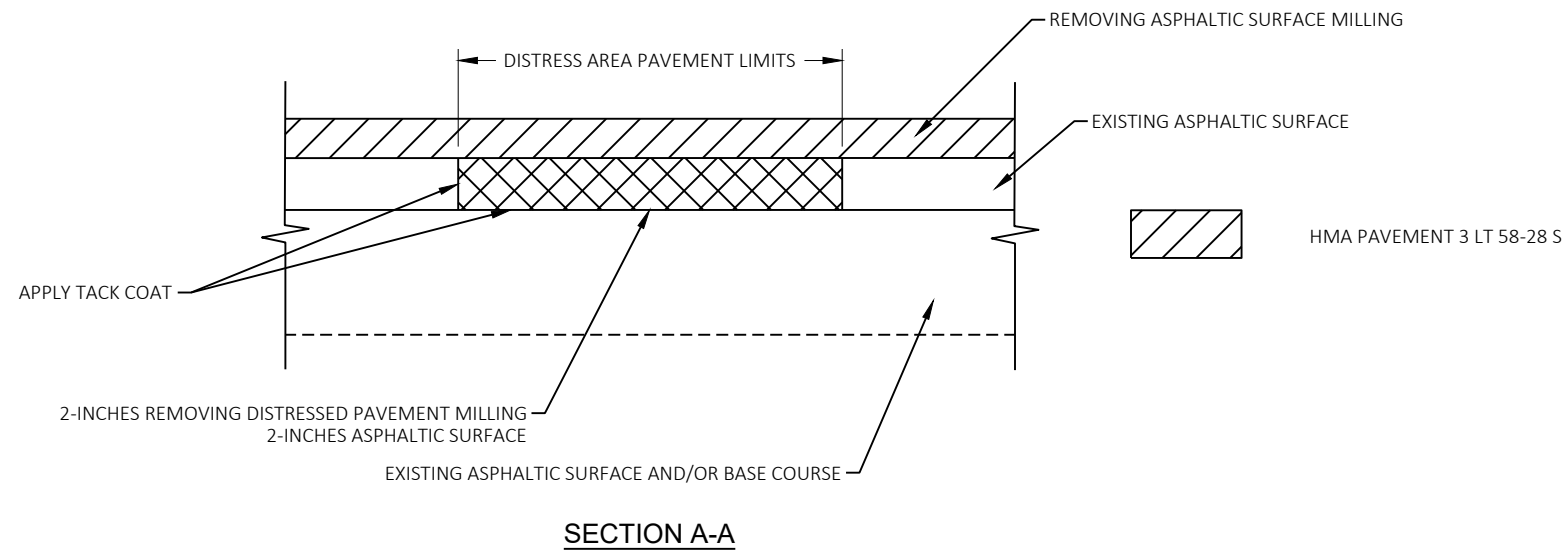
PLAN VIEW
STH 27



SECTION A-A
CONCRETE INTERSECTION GUTTER DETAIL
1ST STREET

GENERAL NOTES

- DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.
- PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.
- THE BOTTOM OF THE GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- FOR STAKING PURPOSES, REFER TO THE CURB RAMP DETAIL SHEETS FOR STH 27 & 1ST STREET (NW AND SW QUADRANTS) FOR FLANGE LINE STA, OFFSET, ELEVATION, NORTHING AND EASTING INFORMATION.
- A** POINT INFORMATION (STA, OFFSET, ELEVATION, NORTHING AND EASTING) IS LOCATED IN THE CURB RAMP DETAIL SHEETS FOR THE STH 27 & 1ST STREET SW QUADRANT.
- B** POINT INFORMATION (STA, OFFSET, ELEVATION, NORTHING AND EASTING) IS LOCATED IN THE CURB RAMP DETAIL SHEETS FOR THE STH 27 & 1ST STREET NW QUADRANT.
- C** NO. 6 TIE BARS SPACED 2'-6" C-C. INSTALLED PERPENDICULAR TO THE FLAG LINE JOINT OF THE CONCRETE CURB & GUTTER 30-INCH TYPE D.
- DRILL TIE BARS INTO CURED AND HARDENED CONCRETE CURB & GUTTER 30-INCH TYPE D SECTIONS AT A DEPTH OF 1/2 THE THICKNESS OF THE NEW CONCRETE INTERSECTION GUTTER TYPE D.
- MAXIMUM DRILL HOLE SIZE IS 1/8" GREATER THAN THE TIE BAR DIAMETER.
- SEE SDD "CONCRETE GUTTER, TIES, AND CURB AND GUTTER APPLICATIONS" FOR ADDITIONAL DETAILS.
- D** MAINTAIN A 4% CROSS SLOPE IN THE 1.8' GUTTER SECTION ADJACENT TO STH 27. ADJUSTMENTS MAY BE MADE AT APPROACHES TO THE CURB & GUTTER 30-INCH TYPE D TIE-INS TO ENSURE POSITIVE WATER FLOW AND/OR TO FIT FIELD CONDITIONS AT THE DISCRETION OF THE FIELD ENGINEER.



NOTE: EXACT LOCATION AND LIMITS OF REMOVING DISTRESSED PAVEMENT MILLING TO BE DETERMINED BY THE ENGINEER IN THE FIELD

REMOVING DISTRESSED PAVEMENT MILLING

RADIUS POINT TABLE

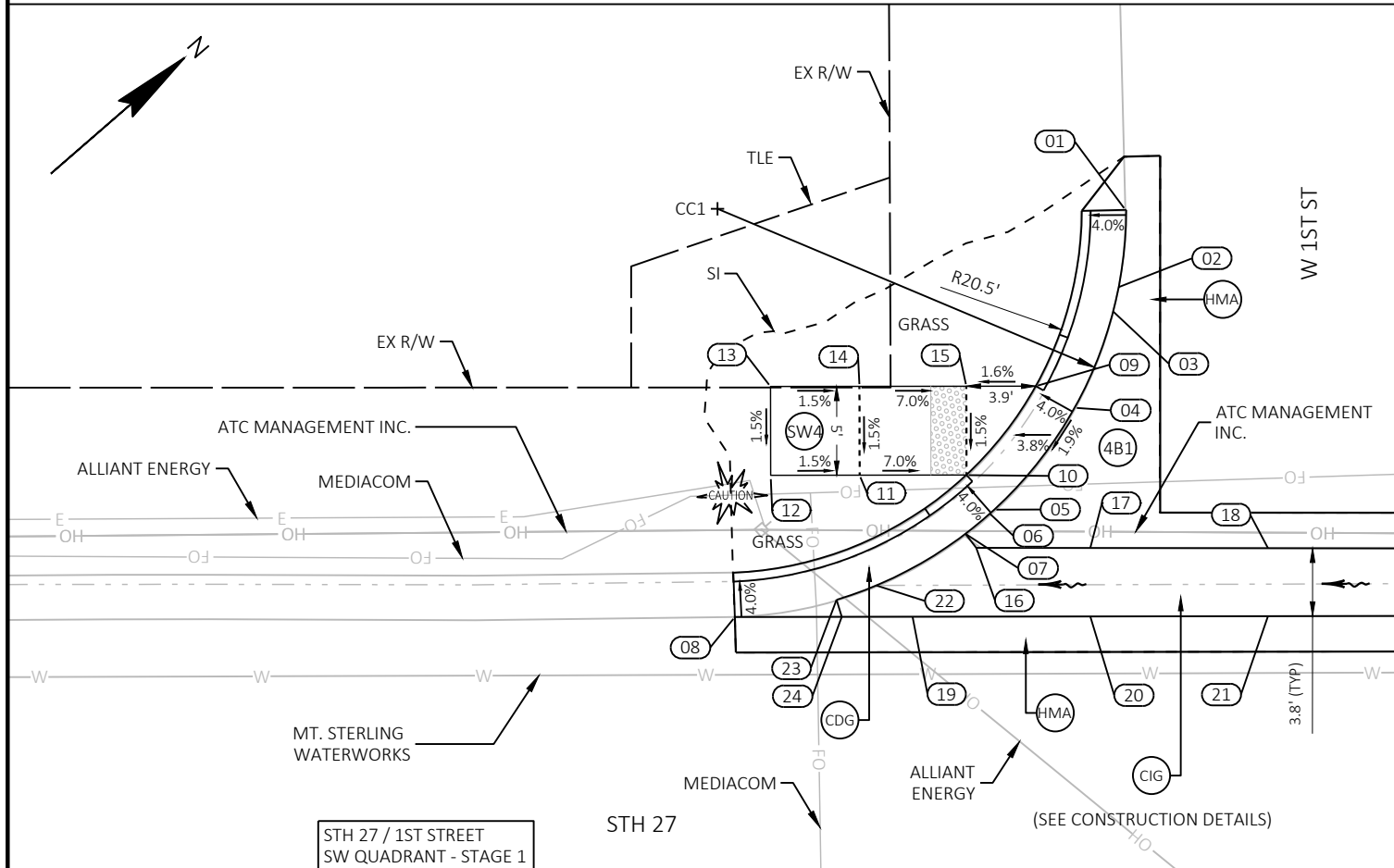
POINT	STATION	OFFSET	RADIUS	COORDINATES
CC1	1185+09.86 - STH 27	43.2' LT	23.0'	Y = 218024.36 X = 375501.80

STH 27 & 1ST STREET - SW QUAD

POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
01	1185+32.81	43.18' LT	1179.80	218041.66	375516.88
02	1185+32.44	38.83' LT	1179.73	218038.52	375519.92
03	1185+32.12	37.46' LT	1179.71	218037.39	375520.75
04	1185+29.95	32.04' LT	1179.59	218032.20	375523.42
05	1185+25.43	26.30' LT	1179.45	218025.02	375524.79
06	1185+25.18	26.08' LT	1179.45	218024.69	375524.80
07	1185+23.81	24.95' LT	1179.41	218022.91	375524.75
08	1185+10.83	20.26' LT	1179.20	218010.03	375519.80
09	1185+27.77	33.26' LT	1179.51	218031.35	375521.07
10	1185+23.87	28.25' LT	1179.38	218025.12	375522.30
11	1185+17.86	28.25' LT	1179.80	218020.57	375518.37
12	1185+12.86	28.15' LT	1179.87	218016.73	375515.16
13	1185+12.84	33.24' LT	1179.95	218020.06	375511.30
14	1185+17.85	33.25' LT	1179.87	218023.84	375514.58
15	1185+23.86	33.25' LT	1179.45	218028.39	375518.52
16	1185+24.42	24.16' LT	1179.44	218022.85	375525.76
17	1185+30.83	24.15' LT	1179.51	218027.69	375529.96
18	1185+40.83	24.14' LT	1179.63	218035.24	375536.52
19	1185+20.83	20.28' LT	1179.42	218017.60	375526.33
20	1185+30.83	20.30' LT	1179.53	218025.17	375532.87

STH 27 & 1ST STREET - SW QUAD

POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
21	1185+40.83	20.31' LT	1179.60	218032.73	375539.41
22	1185+18.79	22.04' LT	1179.31	218017.21	375523.66
23	1185+16.56	21.24' LT	1179.28	218015.00	375522.80
24	1185+16.85	20.28' LT	1179.29	218014.59	375523.73



- NOTES:
1. CONTRACTOR TO VERIFY ELEVATIONS, GRADES, SLOPES, LENGTHS AND MATCH POINTS PRIOR TO CURB RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST ELEVATIONS TO FIT FIELD CONDITIONS.
 3. DASHED LINES SHOWN FOR SIDEWALK ARE FOR INFORMATION ONLY AND DO NOT INDICATE JOINT LOCATIONS.
 4. SEE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS.
 5. ALL STATION AND OFFSET INFORMATION REFERENCE THE STH 27 R/L.
 6. SEE TRAFFIC CONTROL - STAGE 1 PEDESTRIAN ACCOMMODATION FOR ADDITIONAL INFORMATION.

LEGEND

	CURB RAMP TYPE
	CONCRETE SIDEWALK 4-INCH OVER 4" BASE AGGREGATE DENSE 1 1/4-INCH
	CONCRETE CURB & GUTTER 30-INCH TYPE D OVER 11" BASE AGGREGATE DENSE 1 1/4-INCH
	5 1/2" HMA PAVEMENT (1.75" UPPER LAYER, 3.75" LOWER LAYER) OVER 11" BASE AGGREGATE DENSE 1 1/4-INCH
	CONCRETE INTERSECTION GUTTER TYPE D OVER 8.5" BASE AGGREGATE DENSE 1 1/4-INCH
	CURB RAMP DETECTABLE WARNING FIELD (YELLOW)

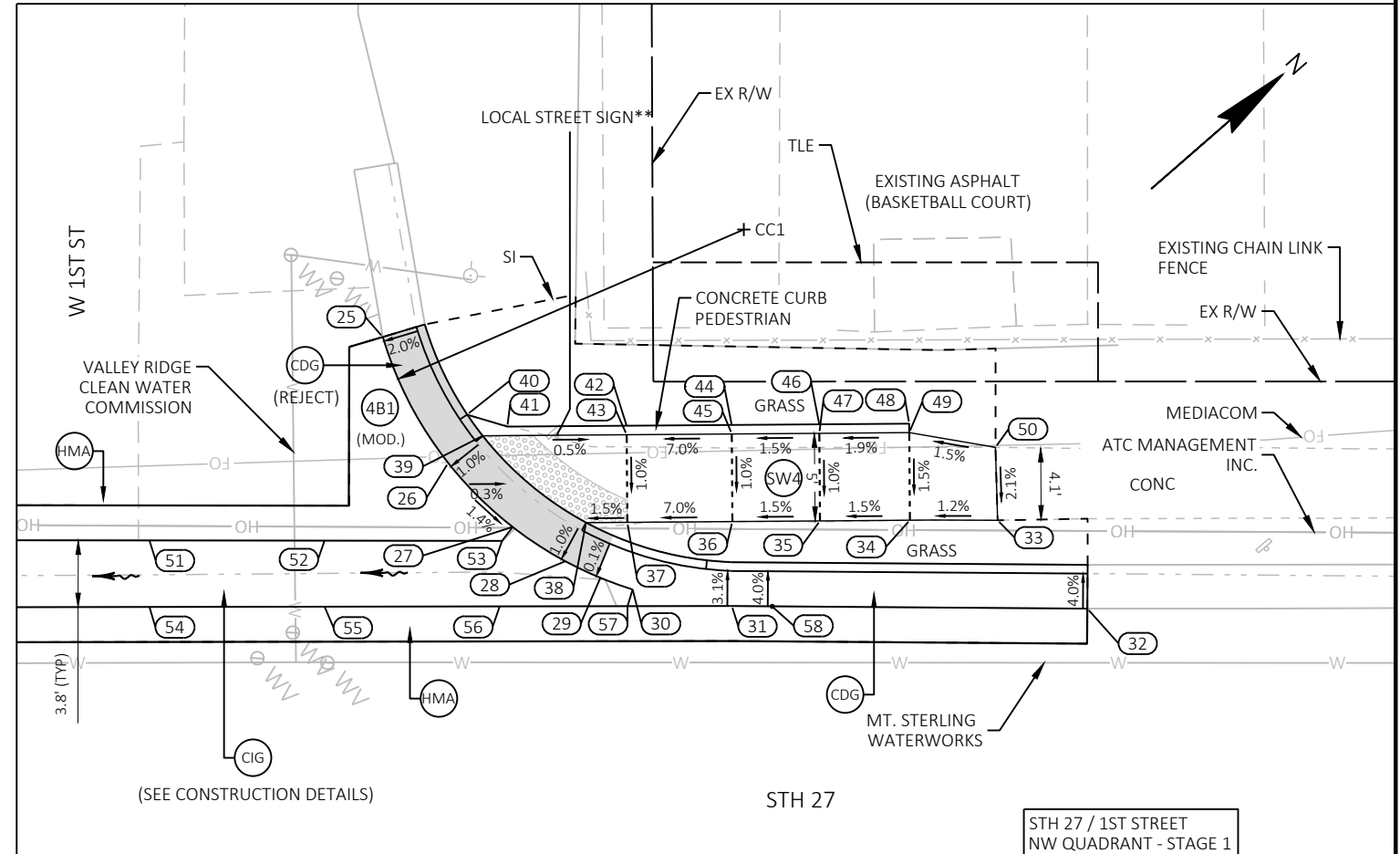
STH 27 & 1ST STREET - NW QUAD					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
25	1185+64.16	35.74' LT	1180.07	218060.46	375543.04
26	1185+67.90	28.55' LT	1179.95	218058.57	375550.93
27	1185+71.58	24.90' LT	1179.88	218058.96	375556.10
28	1185+74.58	22.94' LT	1179.83	218059.94	375559.54
29	1185+76.58	21.99' LT	1179.80	218060.84	375561.57
30	1185+78.43	21.33' LT	1179.82	218061.79	375563.28
31	1185+84.08	20.39' LT	1179.87	218065.45	375567.69
32	1186+04.39	20.23' LT	1180.05	218080.69	375581.12
33	1185+99.27	25.28' LT	1180.51	218080.12	375573.95
34	1185+94.26	25.28' LT	1180.45	218076.34	375570.66
35	1185+89.12	25.25' LT	1180.38	218072.44	375567.33
36	1185+84.12	25.21' LT	1180.30	218068.64	375564.08
37	1185+78.12	25.16' LT	1179.88	218064.08	375560.18
38	1185+75.77	25.14' LT	1179.85	218062.28	375558.66
39	1185+69.86	30.10' LT	1179.98	218061.07	375551.04
40	1185+68.98	31.31' LT	1180.40	218061.20	375549.55
41	1185+71.26	30.62' LT	1180.37	218062.47	375551.57
42	1185+78.08	30.67' LT	1180.43	218067.65	375555.99
43	1185+78.08	30.16' LT	1179.93	218067.32	375556.38
44	1185+84.08	30.72' LT	1180.85	218072.22	375559.89

STH 27 & 1ST STREET - NW QUAD					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
45	1185+84.09	30.21' LT	1180.35	218071.89	375560.28
46	1185+89.08	30.75' LT	1180.93	218076.02	375563.14
47	1185+89.09	30.24' LT	1180.43	218075.69	375563.53
48	1185+94.22	30.79' LT	1180.53	218079.92	375566.48
49	1185+94.22	30.28' LT	1180.53	218079.59	375566.87
50	1185+99.14	29.44' LT	1180.60	218082.76	375570.73
51	1185+50.83	24.13' LT	1179.73	218042.78	375543.09
52	1185+60.83	24.12' LT	1179.84	218050.33	375549.65
53	1185+70.97	24.10' LT	1179.95	218057.98	375556.29
54	1185+50.83	20.33' LT	1179.69	218040.30	375545.95
55	1185+60.83	20.35' LT	1179.71	218047.86	375552.49
56	1185+70.83	20.36' LT	1179.73	218055.43	375559.03
57	1185+78.13	20.37' LT	1179.75	218060.95	375563.81
58	1185+86.39	20.36' LT	1179.89	218067.17	375569.23

RADIUS POINT TABLE				
POINT	STATION	OFFSET	RADIUS	COORDINATES
CC1	1185+84.77 - STH 27	41.9' LT	21.5'	Y = 218080.06 X = 375551.91

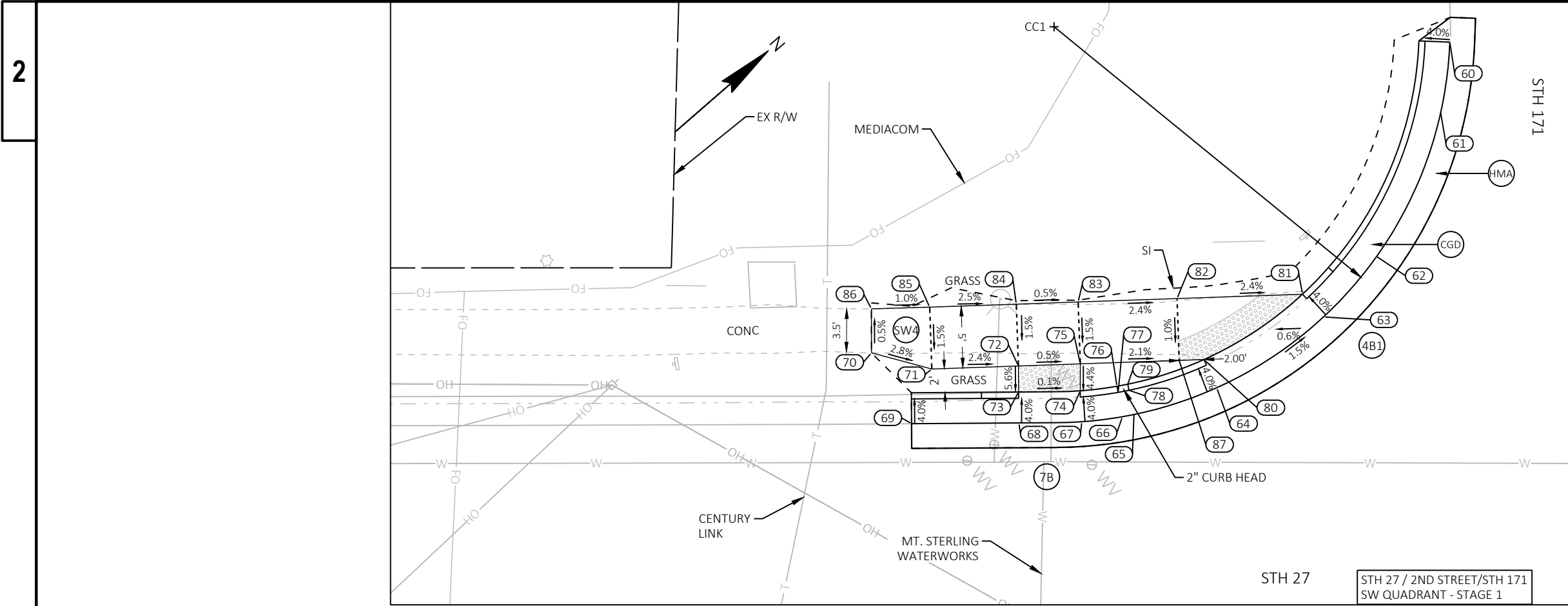
RADIAL DWF FEATURES	
BACK OF CURB RADIUS	19'
LANDING LENGTH 'XR'	8'
RADIAL WARNING FIELD AREA	17.1 SF
RADIAL LONG CHORD	7.8'

LEGEND	
(XXX)	CURB RAMP TYPE
(SW4)	CONCRETE SIDEWALK 4-INCH OVER 4" BASE AGGREGATE DENSE 1 3/4-INCH
(CGD)	CONCRETE CURB & GUTTER 30-INCH TYPE D OVER 11" BASE AGGREGATE DENSE 1 3/4-INCH
(HMA)	5 1/2" HMA PAVEMENT (1.75" UPPER LAYER, 3.75" LOWER LAYER) OVER 11" BASE AGGREGATE DENSE 1 3/4-INCH
(CGI)	CONCRETE INTERSECTION GUTTER TYPE D OVER 8.5" BASE AGGREGATE DENSE 1 3/4-INCH
(Yellow Box)	CURB RAMP DETECTABLE WARNING FIELD (YELLOW)



**NOTE: CONTACT VILLAGE OF MT STERLING TO HAVE THE MUNICIPALITY MOVE THIS SIGN PRIOR TO CONSTRUCTION.

- NOTES:
1. CONTRACTOR TO VERIFY ELEVATIONS, GRADES, SLOPES, LENGTHS AND MATCH POINTS PRIOR TO CURB RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST ELEVATIONS TO FIT FIELD CONDITIONS.
 3. DASHED LINES SHOWN FOR SIDEWALK ARE FOR INFORMATION ONLY AND DO NOT INDICATE JOINT LOCATIONS.
 4. SEE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS.
 5. ALL STATION AND OFFSET INFORMATION REFERENCE THE STH 27 R/L.
 6. SEE TRAFFIC CONTROL - STAGE 1 PEDESTRIAN ACCOMMODATION FOR ADDITIONAL INFORMATION.



LEGEND	
(XXX)	CURB RAMP TYPE
(SW4)	CONCRETE SIDEWALK 4-INCH OVER 4" BASE AGGREGATE DENSE 1 1/4-INCH
(CGD)	CONCRETE CURB & GUTTER 30-INCH TYPE D OVER 11" BASE AGGREGATE DENSE 1 1/4-INCH
(HMA)	5 1/2" HMA PAVEMENT (1.75" UPPER LAYER, 3.75" LOWER LAYER) OVER 11" BASE AGGREGATE DENSE 1 1/4-INCH
(Yellow Box)	CURB RAMP DETECTABLE WARNING FIELD (YELLOW)

RADIUS POINT TABLE				
POINT	STATION	OFFSET	RADIUS	COORDINATES
CC1	1188+05.64 - STH 27 100+52.72 - STH 171	52.7' LT 49.2' LT	32.0'	Y = 218254.00 X = 375688.45

**NOTE: TERRACE IS GRASSED AND 2" CURB HEAD BETWEEN POINTS 76 AND 79.

RADIAL DWF FEATURES	
BACK OF CURB RADIUS	29.5'
LANDING LENGTH 'XR'	10.1'
RADIAL WARNING FIELD AREA	20.1 SF
RADIAL LONG CHORD	9.6'

STH 27 & 2ND STREET - SW QUAD					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
60	1188+37.61	51.47' LT	1180.35	218277.33	375710.35
61	1188+36.87	45.76' LT	1180.64	218273.03	375714.17
62	1188+31.70	34.15' LT	1180.98	218261.51	375719.55
63	1188+27.41	29.27' LT	1181.08	218255.08	375720.43
64	1188+18.77	23.54' LT	1181.24	218244.80	375719.10
65	1188+12.10	21.38' LT	1181.34	218238.34	375716.36
66	1188+11.16	21.20' LT	1181.34	218237.52	375715.88
67	1188+07.89	20.80' LT	1181.34	218234.78	375714.04
68	1188+02.79	20.70' LT	1181.34	218230.87	375710.77
69	1187+94.13	20.65' LT	1181.34	218224.29	375705.14
70	1187+90.89	26.36' LT	1181.71	218225.58	375698.69
71	1187+95.77	25.06' LT	1181.57	218228.42	375702.88
72	1188+02.76	25.30' LT	1181.39	218233.86	375707.28
73	1188+02.77	23.20' LT	1181.28	218232.49	375708.87
74	1188+07.75	23.30' LT	1181.27	218236.32	375712.06
75	1188+07.76	25.48' LT	1181.37	218237.75	375710.42
76	1188+10.73	23.67' LT	1181.43	218238.81	375713.73
77	1188+10.81	23.17' LT	1181.26	218238.55	375714.16
78	1188+11.69	23.34' LT	1181.26	218239.32	375714.61
79	1188+11.59	23.83' LT	1181.43	218239.57	375714.18

STH 27 & 2ND STREET - SW QUAD					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
80	1188+17.75	25.82' LT	1181.16	218245.52	375716.70
81	1188+25.71	31.10' LT	1181.00	218255.00	375717.93
82	1188+15.58	30.75' LT	1181.25	218247.11	375711.56
83	1188+07.58	30.47' LT	1181.44	218240.89	375706.53
84	1188+02.59	30.30' LT	1181.47	218237.00	375703.39
85	1187+95.59	30.06' LT	1181.64	218231.56	375698.99
86	1187+90.89	29.92' LT	1181.69	218227.91	375696.01
87	1188+15.75	25.75' LT	1181.20	218243.97	375715.45

NOTES:
 1. CONTRACTOR TO VERIFY ELEVATIONS, GRADES, SLOPES, LENGTHS AND MATCH POINTS PRIOR TO CURB RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST ELEVATIONS TO FIT FIELD CONDITIONS.
 3. DASHED LINES SHOWN FOR SIDEWALK ARE FOR INFORMATION ONLY AND DO NOT INDICATE JOINT LOCATIONS.
 4. SEE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS.
 5. ALL STATION AND OFFSET INFORMATION REFERENCE THE STH 27 R/L.
 6. SEE TRAFFIC CONTROL - STAGE 1 PEDESTRIAN ACCOMMODATION FOR ADDITIONAL INFORMATION.

STH 27 & 2ND STREET - NW QUAD					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
145	1188+71.82	44.08' LT	1180.64	218298.33	375738.34
146	1188+76.84	29.68' LT	1181.09	218292.69	375752.51
147	1188+78.04	28.25' LT	1181.12	218292.66	375754.37
148	1188+85.62	22.67' LT	1181.26	218294.73	375763.56
149	1188+86.21	22.40' LT	1181.27	218295.00	375764.15
150	1188+90.97	20.89' LT	1181.37	218297.61	375768.41
151	1188+94.94	20.42' LT	1181.45	218300.29	375771.37
152	1188+97.53	20.41' LT	1181.50	218302.25	375773.07
153	1189+02.94	20.39' LT	1181.55	218306.32	375776.63
154	1189+23.00	20.33' LT	1181.70	218321.43	375789.82
155	1189+41.02	20.27' LT	1181.68	218335.00	375801.67
156	1189+46.01	20.26' LT	1181.68	218338.76	375804.95
157	1189+52.46	20.24' LT	1181.67	218343.62	375809.19
158	1189+46.02	22.76' LT	1181.60	218340.41	375803.07
159	1189+41.02	22.77' LT	1181.60	218336.65	375799.78
160	1189+02.95	22.38' LT	1181.47	218307.63	375775.13
161	1189+02.95	22.88' LT	1181.97	218307.96	375774.75
163	1188+97.54	22.40' LT	1181.43	218303.55	375771.58
164	1188+97.54	22.90' LT	1181.59	218303.88	375771.20
165	1188+91.37	22.85' LT	1181.29	218299.19	375767.20

STH 27 & 2ND STREET - NW QUAD					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
166	1188+91.47	23.34' LT	1181.46	218299.59	375766.89
167	1188+86.68	24.93' LT	1181.18	218297.01	375762.55
168	1188+79.89	29.94' LT	1181.04	218295.16	375754.32
169	1188+76.92	34.11' LT	1181.40	218295.65	375749.22
170	1188+89.72	29.94' LT	1181.19	218302.59	375760.75
171	1188+89.72	24.94' LT	1181.21	218299.32	375764.53
172	1188+95.72	29.94' LT	1181.61	218307.12	375764.69
173	1188+95.72	24.94' LT	1181.56	218303.85	375768.46
174	1189+00.72	29.94' LT	1181.68	218310.90	375767.97
175	1189+00.72	24.94' LT	1181.63	218307.62	375771.74
176	1189+05.44	29.93' LT	1182.00	218314.46	375771.06
177	1189+05.44	24.94' LT	1181.93	218311.18	375774.83
178	1189+10.44	29.93' LT	1182.05	218318.24	375774.34
179	1189+10.44	24.94' LT	1181.97	218314.96	375778.11
180	1189+17.57	29.93' LT	1182.20	218323.62	375779.01
181	1189+17.61	24.93' LT	1182.13	218320.37	375782.81
182	1189+27.60	29.85' LT	1182.26	218331.14	375785.64
183	1189+27.60	24.86' LT	1182.19	218327.87	375789.41
184	1189+41.04	29.82' LT	1181.76	218341.28	375794.47
185	1189+41.03	24.84' LT	1181.69	218338.00	375798.23

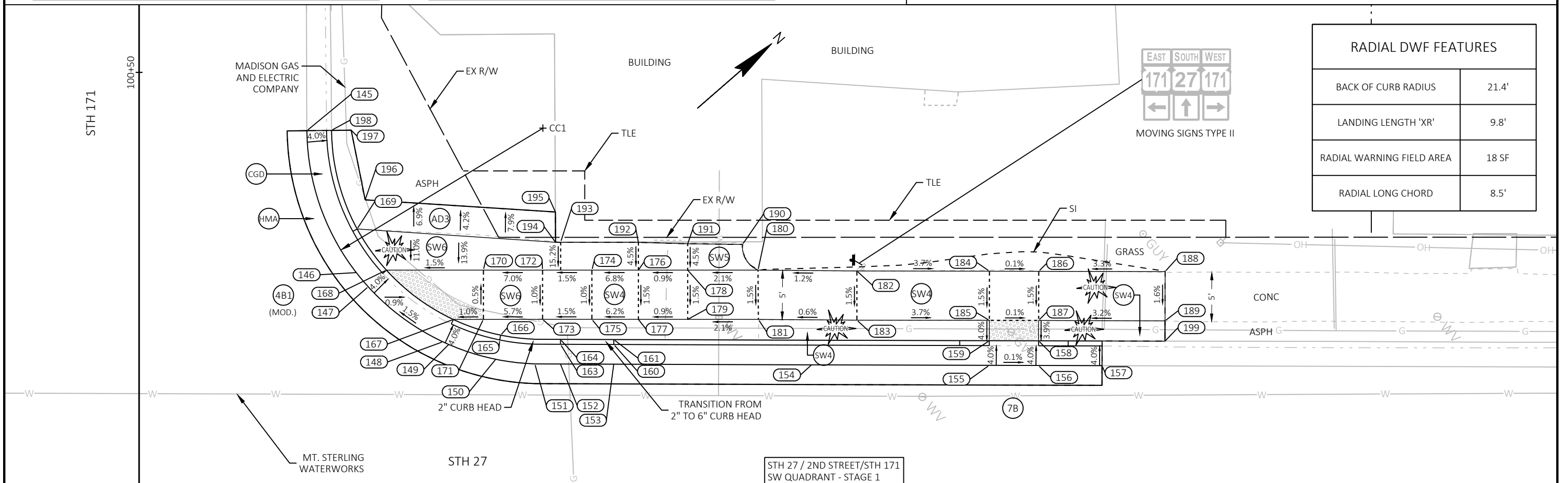
STH 27 & 2ND STREET - NW QUAD					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
186	1189+46.04	29.83' LT	1181.76	218345.06	375797.74
187	1189+46.03	24.83' LT	1181.68	218341.77	375801.51
188	1189+58.85	29.79' LT	1182.17	218354.71	375806.17
189	1189+58.84	24.79' LT	1182.10	218351.42	375809.94
190	1189+15.99	32.54' LT	1182.21	218324.14	375776.00
191	1189+10.44	32.61' LT	1182.17	218319.99	375772.32
192	1189+05.44	32.75' LT	1182.13	218316.30	375768.94
193	1188+97.56	32.79' LT	1182.06	218310.38	375763.74
194	1188+97.04	32.76' LT	1182.06	218309.96	375763.42
195	1188+97.02	35.83' LT	1181.81	218311.96	375761.09
196	1188+77.73	37.06' LT	1181.12	218298.20	375747.52
197	1188+76.32	44.13' LT	1180.99	218301.76	375741.26
198	1188+74.30	44.11' LT	1181.06	218300.22	375739.95
199	1189+58.84	22.73' LT	1181.97	218350.08	375811.49

RADIUS POINT TABLE				
POINT	STATION	OFFSET	RADIUS	COORDINATES
CC1	1188+95.76 - STH 27 100+44.43 - STH 171	44.3' LT 40.9' RT	23.9'	Y = 218316.59 X = 375753.83

LEGEND

- (XXX) CURB RAMP TYPE
- (SW4) CONCRETE SIDEWALK 4-INCH OVER 4" BASE AGGREGATE DENSE 1 1/4-INCH
- (SW6) CONCRETE SIDEWALK 6-INCH OVER 4" BASE AGGREGATE DENSE 1 1/4-INCH
- (AD3) 3" ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES OVER 3" BASE AGGREGATE DENSE 1 1/4-INCH
- (CGD) CONCRETE CURB & GUTTER 30-INCH TYPE D OVER 9.5" BASE AGGREGATE DENSE 1 1/4-INCH
- (HMA) 5 1/2" HMA PAVEMENT (1.75" UPPER LAYER, 3.75" LOWER LAYER) OVER 9.5" BASE AGGREGATE DENSE 1 1/4-INCH
- (Yellow Box) CURB RAMP DETECTABLE WARNING FIELD (YELLOW)

- NOTES:**
- CONTRACTOR TO VERIFY ELEVATIONS, GRADES, SLOPES, LENGTHS AND MATCH POINTS PRIOR TO CURB RAMP AND SIDEWALK CONSTRUCTION.
 - THE ENGINEER MAY ADJUST ELEVATIONS TO FIT FIELD CONDITIONS.
 - DASHED LINES SHOWN FOR SIDEWALK ARE FOR INFORMATION ONLY AND DO NOT INDICATE JOINT LOCATIONS.
 - SEE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS.
 - ALL STATION AND OFFSET INFORMATION REFERENCE THE STH 27 R/L.
 - SEE TRAFFIC CONTROL - STAGE 1 PEDESTRIAN ACCOMMODATION FOR ADDITIONAL INFORMATION.



PROJECT NO: 5540-02-71	HWY: STH 27	COUNTY: CRAWFORD	CURB RAMP DETAILS	SHEET	E
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RADIUS POINT TABLE				
POINT	STATION	OFFSET	RADIUS	COORDINATES
CC1	1188+04.97 - STH 27 99+70.11 - STH 171	42.2' RT 53.0' LT	22.6'	Y = 218191.32 X = 375759.69

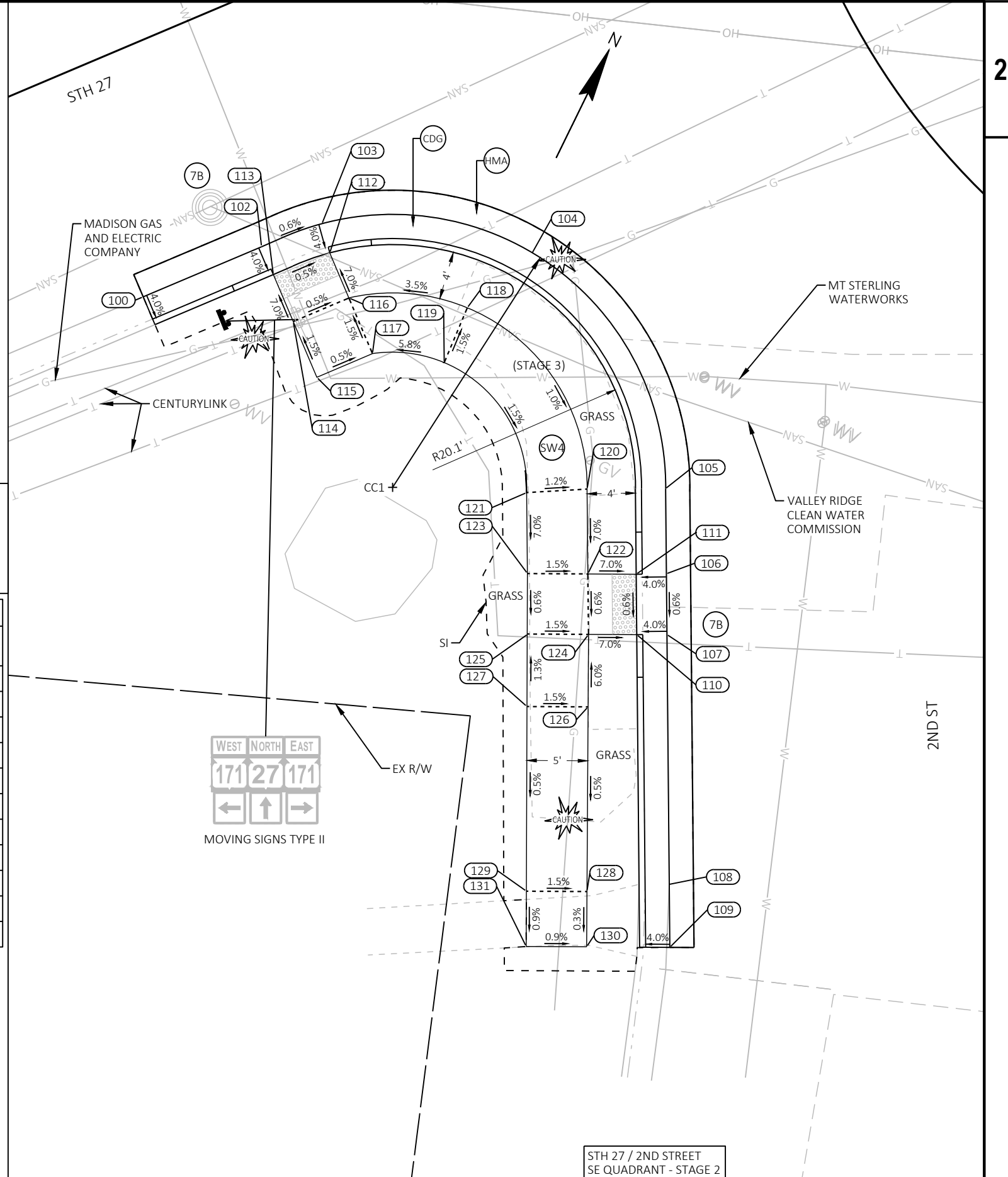
LEGEND

- (XXX) CURB RAMP TYPE
- (SW4) CONCRETE SIDEWALK 4-INCH OVER 4" BASE AGGREGATE DENSE 1 1/4-INCH
- (CGD) CONCRETE CURB & GUTTER 30-INCH TYPE D OVER 11" BASE AGGREGATE DENSE 1 1/4-INCH
- (HMA) 5 1/2" HMA PAVEMENT (1.75" UPPER LAYER, 3.75" LOWER LAYER) OVER 11" BASE AGGREGATE DENSE 1 1/4-INCH
- [Pattern] CURB RAMP DETECTABLE WARNING FIELD (YELLOW)

- NOTES:**
- CONTRACTOR TO VERIFY ELEVATIONS, GRADES, SLOPES, LENGTHS AND MATCH POINTS PRIOR TO CURB RAMP AND SIDEWALK CONSTRUCTION.
 - THE ENGINEER MAY ADJUST ELEVATIONS TO FIT FIELD CONDITIONS.
 - DASHED LINES SHOWN FOR SIDEWALK ARE FOR INFORMATION ONLY AND DO NOT INDICATE JOINT LOCATIONS.
 - SEE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS.
 - ALL STATION AND OFFSET INFORMATION REFERENCE THE STH 27 R/L.
 - SEE TRAFFIC CONTROL - STAGE 1 PEDESTRIAN ACCOMMODATION FOR ADDITIONAL INFORMATION.

STH 27 & 2ND STREET - SE QUAD					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
100	1187+92.12	19.60' RT	1181.52	218196.40	375734.22
102	1188+02.75	19.60' RT	1181.45	218204.43	375741.19
103	1188+08.08	19.81' RT	1181.42	218208.32	375744.84
104	1188+23.16	28.80' RT	1181.31	218213.81	375761.51
105	1188+25.68	51.07' RT	1181.16	218201.13	375779.98
106	1188+23.01	57.57' RT	1181.12	218194.85	375783.15
107	1188+21.10	62.20' RT	1181.09	218190.38	375785.39
108	1188+13.23	81.33' RT	1180.96	218171.89	375794.69
109	1188+11.24	86.15' RT	1180.92	218167.24	375797.02
110	1188+18.81	61.26' RT	1181.01	218189.27	375783.18
111	1188+20.72	56.64' RT	1181.04	218193.74	375780.94
112	1188+07.75	22.27' RT	1181.36	218206.45	375746.48
113	1188+02.75	22.10' RT	1181.37	218202.79	375743.08
114	1188+02.75	26.32' RT	1181.67	218200.02	375746.27
115	1188+02.75	31.33' RT	1181.75	218196.74	375750.05
116	1188+07.75	26.32' RT	1181.64	218203.80	375749.54
117	1188+07.75	31.32' RT	1181.72	218200.53	375753.32
118	1188+16.45	30.89' RT	1181.99	218207.37	375758.69
119	1188+12.92	34.33' RT	1182.06	218202.46	375758.98
120	1188+19.73	48.56' RT	1181.81	218198.27	375774.19

STH 27 & 2ND STREET - SE QUAD					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
121	1188+15.01	46.88' RT	1181.86	218195.81	375769.83
122	1188+17.05	55.05' RT	1181.31	218192.00	375777.34
123	1188+12.46	53.08' RT	1181.40	218189.83	375772.84
124	1188+15.15	59.68' RT	1181.28	218187.53	375779.59
125	1188+10.56	57.70' RT	1181.36	218185.36	375775.09
126	1188+12.78	65.19' RT	1181.64	218182.13	375782.20
127	1188+08.13	63.19' RT	1181.71	218179.93	375777.64
128	1188+06.73	79.23' RT	1181.57	218168.37	375788.84
129	1188+02.13	77.24' RT	1181.64	218166.19	375784.32
130	1188+04.92	83.43' RT	1181.55	218164.25	375790.83
131	1188+00.33	81.45' RT	1181.60	218162.07	375786.32



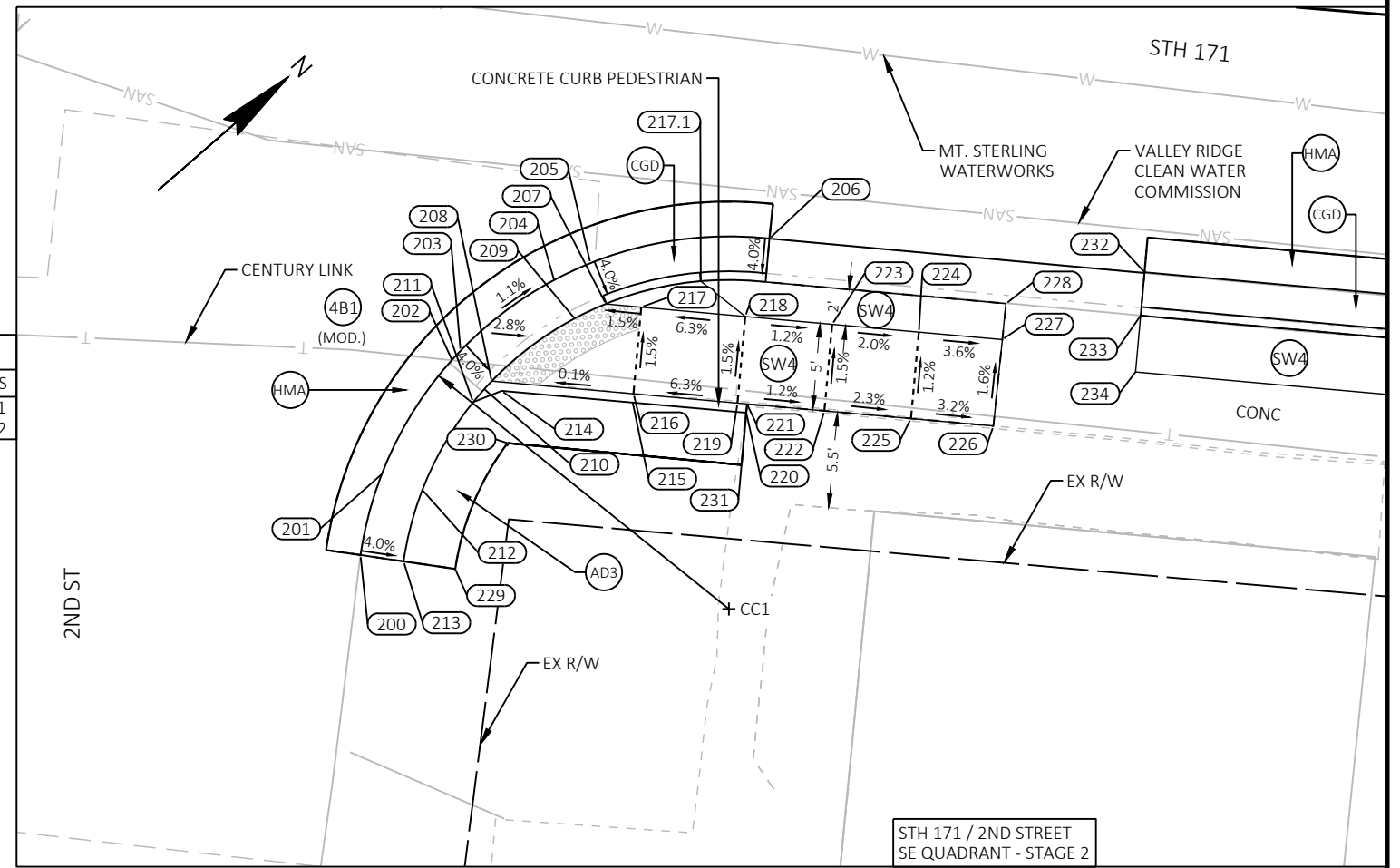
STH 171 & 2ND STREET - SE QUAD					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
200	99+34.53	39.12' LT	1181.01	218193.18	375823.06
201	99+34.78	34.34' LT	1181.09	218197.89	375822.11
202	99+33.69	26.82' LT	1181.15	218205.55	375822.85
203	99+33.48	26.16' LT	1181.15	218206.24	375823.06
204	99+30.65	20.69' LT	1181.09	218212.25	375826.10
205	99+29.33	19.22' LT	1181.05	218214.04	375827.54
206	99+21.72	15.88' LT	1180.88	218219.83	375836.16
207	99+28.26	21.27' LT	1180.98	218212.33	375829.37
208	99+31.94	27.29' LT	1181.08	218205.45	375825.43
209	99+29.39	22.52' LT	1181.01	218210.79	375828.12
210	99+32.12	27.84' LT	1181.07	218204.87	375825.25
211	99+32.38	28.74' LT	1181.57	218203.91	375825.01
212	99+33.19	34.44' LT	1181.01	218198.10	375824.60
213	99+33.04	38.67' LT	1180.93	218193.94	375825.44
214	99+31.45	27.65' LT	1181.35	218205.21	375826.19
215	99+26.31	26.46' LT	1181.45	218207.92	375833.25
216	99+26.35	25.96' LT	1181.08	218208.39	375833.06
217	99+26.77	21.00' LT	1181.01	218213.06	375831.27
217.1	99+24.46	18.89' LT	1181.37	218215.90	375833.69
218	99+22.32	20.49' LT	1181.38	218215.21	375836.88

STH 171 & 2ND STREET - SE QUAD					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
219	99+22.16	25.49' LT	1181.46	218210.54	375838.67
220	99+21.76	25.96' LT	1181.43	218210.27	375839.36
221	99+21.77	25.47' LT	1181.43	218210.72	375839.18
222	99+18.40	25.40' LT	1181.36	218212.34	375843.34
223	99+18.38	20.41' LT	1181.33	218217.00	375841.56
224	99+13.39	20.43' LT	1181.22	218218.79	375846.22
225	99+13.41	25.42' LT	1181.26	218214.12	375848.00
226	99+08.66	25.44' LT	1181.13	218215.83	375852.44
227	99+08.56	20.45' LT	1181.05	218220.51	375850.72
228	99+08.54	18.40' LT	1180.98	218222.44	375850.00
229	99+31.23	38.21' LT	1181.24	218194.86	375828.29
230	99+30.67	30.44' LT	1181.31	218202.66	375827.81
231	99+21.68	28.97' LT	1181.48	218207.46	375840.44
232	99+00.76	15.91' LT	1180.33	218227.58	375856.35
233	99+00.77	18.41' LT	1180.70	218225.24	375857.24
234	99+00.79	21.66' LT	1180.82	218222.21	375858.41

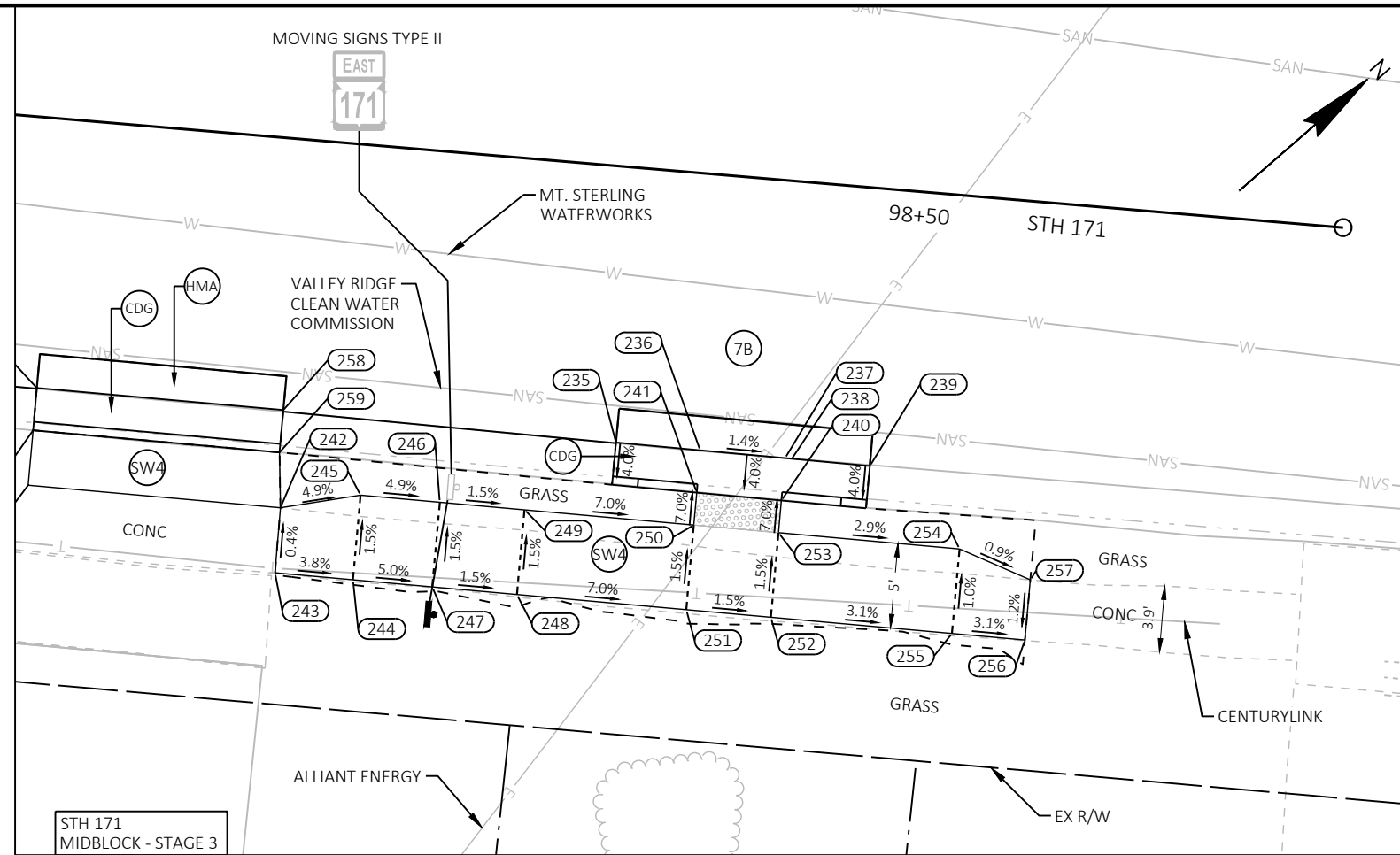
RADIUS POINT TABLE				
POINT	STATION	OFFSET	RADIUS	COORDINATES
CC1	1188+66.18 - STH 27 99+21.48 - STH 171	99.9' RT 37.3' LT	21.4'	Y = 218199.71 X = 375843.42

RADIAL DWF FEATURES	
BACK OF CURB RADIUS	18.9'
LANDING LENGTH 'XR'	8.2'
RADIAL WARNING FIELD AREA	16 SF
RADIAL LONG CHORD	8'

LEGEND	
(XXX)	CURB RAMP TYPE
(SW4)	CONCRETE SIDEWALK 4-INCH OVER 4" BASE AGGREGATE DENSE 1 1/4-INCH
(AD3)	3" ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES OVER 3" BASE AGGREGATE DENSE 1 1/4-INCH
(CGD)	CONCRETE CURB & GUTTER 30-INCH TYPE D OVER 9.5" BASE AGGREGATE DENSE 1 1/4-INCH
(HMA)	5 1/2" HMA PAVEMENT (1.75" UPPER LAYER, 3.75" LOWER LAYER) OVER 9.5" BASE AGGREGATE DENSE 1 1/4-INCH
(Yellow Box)	CURB RAMP DETECTABLE WARNING FIELD (YELLOW)



- NOTES:
1. CONTRACTOR TO VERIFY ELEVATIONS, GRADES, SLOPES, LENGTHS AND MATCH POINTS PRIOR TO CURB RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST ELEVATIONS TO FIT FIELD CONDITIONS.
 3. DASHED LINES SHOWN FOR SIDEWALK ARE FOR INFORMATION ONLY AND DO NOT INDICATE JOINT LOCATIONS.
 4. SEE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS.
 5. ALL STATION AND OFFSET INFORMATION REFERENCE THE STH 171 R/L.
 6. SEE TRAFFIC CONTROL - STAGE 2 PEDESTRIAN ACCOMMODATION FOR ADDITIONAL INFORMATION.



LEGEND

- (XXX) CURB RAMP TYPE
- (SW4) CONCRETE SIDEWALK 4-INCH OVER 4" BASE AGGREGATE DENSE 1 1/4-INCH
- (CGD) CONCRETE CURB & GUTTER 30-INCH TYPE D OVER 9.5" BASE AGGREGATE DENSE 1 1/4-INCH
- (HMA) 5 1/2" HMA PAVEMENT (1.75" UPPER LAYER, 3.75" LOWER LAYER) OVER 9.5" BASE AGGREGATE DENSE 1 1/4-INCH
- [Pattern] CURB RAMP DETECTABLE WARNING FIELD (YELLOW)

STH 171 MIDBLOCK - STAGE 3

STH 171 MIDBLOCK					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
235	98+66.54	16.22' LT	1179.28	218239.68	375888.35
236	98+61.57	16.26' LT	1179.16	218241.45	375893.00
237	98+56.56	16.29' LT	1179.09	218243.23	375897.68
238	98+56.16	16.30' LT	1179.08	218243.38	375898.06
239	98+51.55	16.33' LT	1178.63	218245.01	375902.37
240	98+56.56	18.79' LT	1179.01	218240.90	375898.59
241	98+61.56	18.76' LT	1179.08	218239.13	375893.91
242	98+85.89	21.72' LT	1180.46	218227.55	375872.32
243	98+85.88	25.56' LT	1180.47	218223.97	375873.71
244	98+81.28	25.58' LT	1180.30	218225.62	375878.01
245	98+81.25	20.58' LT	1180.22	218230.29	375876.22
246	98+76.57	20.60' LT	1179.99	218231.97	375880.59
247	98+76.60	25.60' LT	1180.06	218227.30	375882.38
248	98+71.60	25.62' LT	1179.99	218229.09	375887.05
249	98+71.57	20.62' LT	1179.91	218233.76	375885.26
250	98+61.57	20.67' LT	1179.21	218237.34	375894.60
251	98+61.60	25.67' LT	1179.29	218232.67	375896.39
252	98+56.60	25.70' LT	1179.22	218234.46	375901.06
253	98+56.57	20.70' LT	1179.14	218239.13	375899.27

STH 171 MIDBLOCK					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
254	98+45.88	20.75' LT	1178.84	218242.95	375909.25
255	98+45.90	25.75' LT	1178.89	218238.29	375911.04
256	98+41.60	25.77' LT	1178.75	218239.83	375915.06
257	98+41.58	22.21' LT	1178.80	218243.15	375913.79
258	98+86.20	15.98' LT	1179.92	218232.79	375869.94
259	98+86.21	18.48' LT	1180.35	218230.45	375870.84

- NOTES:**
1. CONTRACTOR TO VERIFY ELEVATIONS, GRADES, SLOPES, LENGTHS AND MATCH POINTS PRIOR TO CURB RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST ELEVATIONS TO FIT FIELD CONDITIONS.
 3. DASHED LINES SHOWN FOR SIDEWALK ARE FOR INFORMATION ONLY AND DO NOT INDICATE JOINT LOCATIONS.
 4. SEE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS.
 5. ALL STATION AND OFFSET INFORMATION REFERENCE THE STH 171 R/L.
 6. SEE TRAFFIC CONTROL - STAGE 3 PEDESTRIAN ACCOMMODATION FOR ADDITIONAL INFORMATION.

RADIUS POINT TABLE

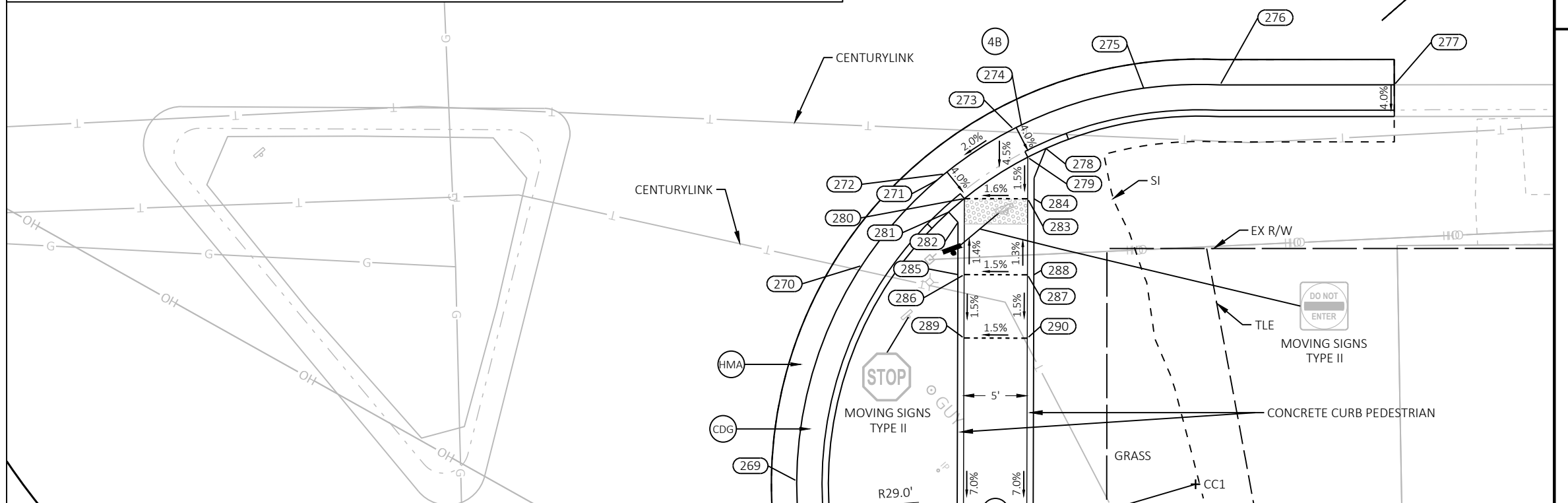
POINT	STATION	OFFSET	RADIUS	COORDINATES
CC1	1189+59.14 - STH 27 98+63.36 - STH 171	51.3' RT 49.2' RT	31.5'	Y = 218301.78 X = 375867.62

NOTES:

1. CONTRACTOR TO VERIFY ELEVATIONS, GRADES, SLOPES, LENGTHS AND MATCH POINTS PRIOR TO CURB RAMP AND SIDEWALK CONSTRUCTION.
2. THE ENGINEER MAY ADJUST ELEVATIONS TO FIT FIELD CONDITIONS.
3. DASHED LINES SHOWN FOR SIDEWALK ARE FOR INFORMATION ONLY AND DO NOT INDICATE JOINT LOCATIONS.
4. SEE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS.
5. ALL STATION AND OFFSET INFORMATION REFERENCE THE STH 27 R/L.
6. SEE TRAFFIC CONTROL - STAGE 1 PEDESTRIAN ACCOMMODATION FOR ADDITIONAL INFORMATION.

LEGEND

- (XXX) CURB RAMP TYPE
- (SW4) CONCRETE SIDEWALK 4-INCH OVER 4" BASE AGGREGATE DENSE 1 1/4-INCH
- (CGD) CONCRETE CURB & GUTTER 30-INCH TYPE D OVER 9.5" BASE AGGREGATE DENSE 1 1/4-INCH
- (HMA) 5 1/2" HMA PAVEMENT (1.75" UPPER LAYER, 3.75" LOWER LAYER) OVER 9.5" BASE AGGREGATE DENSE 1 1/4-INCH
- (Yellow Box) CURB RAMP DETECTABLE WARNING FIELD (YELLOW)



STH 27 & STH 171 - NE QUAD

POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
265	98+53.21	17.66' RT	1178.60	218276.09	375888.50
266	98+63.21	17.66' RT	1178.97	218272.47	375879.18
267	98+69.67	18.30' RT	1179.10	218270.73	375872.93
268	98+80.76	22.91' RT	1179.72	218271.00	375860.92
269	98+91.34	34.68' RT	1180.20	218278.14	375846.80
270	98+94.73	52.11' RT	1180.75	218293.16	375837.32
271	98+92.62	60.83' RT	1181.02	218302.05	375836.13
272	98+92.33	61.55' RT	1181.04	218302.83	375836.14
273	98+89.17	67.23' RT	1181.17	218309.26	375837.03
274	98+88.75	67.81' RT	1181.18	218309.96	375837.20
275	98+81.58	74.87' RT	0.00	218319.14	375841.33
276	98+76.36	77.92' RT	1181.64	218323.87	375845.09
277	98+64.20	84.34' RT	0.00	218334.26	375854.10
278	98+86.18	67.06' RT	1181.36	218310.19	375839.87
279	98+87.11	65.78' RT	1181.09	218308.65	375839.47
280	98+90.03	60.57' RT	1180.96	218302.74	375838.64
281	98+90.63	59.05' RT	1181.15	218301.11	375838.63
282	98+89.60	58.67' RT	1181.17	218301.13	375839.73
283	98+85.60	62.89' RT	1181.04	218306.51	375841.93
284	98+85.16	63.12' RT	1181.54	218306.89	375842.25

STH 27 & STH 171 - NE QUAD

POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
285	98+87.69	55.02' RT	1181.54	218298.42	375842.83
286	98+87.24	55.25' RT	1181.04	218298.80	375843.16
287	98+82.82	57.57' RT	1181.11	218302.57	375846.45
288	98+82.37	57.81' RT	1181.61	218302.95	375846.78
289	98+84.92	50.83' RT	1180.96	218295.51	375846.93
290	98+80.49	53.14' RT	1181.04	218299.28	375850.21
291	98+74.27	30.50' RT	1179.36	218280.42	375864.22
292	98+69.84	32.82' RT	1179.43	218284.19	375867.51
293	98+72.39	25.83' RT	1179.78	218276.76	375867.66
294	98+71.95	26.07' RT	1179.28	218277.14	375867.99
295	98+67.52	28.39' RT	1179.36	218280.91	375871.28
296	98+67.08	28.62' RT	1179.86	218281.28	375871.61
297	98+70.67	22.54' RT	1179.36	218274.31	375870.46
298	98+71.04	21.20' RT	1179.40	218272.93	375870.61
299	98+69.16	20.75' RT	1179.02	218273.19	375872.51
300	98+64.74	23.07' RT	1178.94	218276.96	375875.80
301	98+64.29	23.30' RT	1179.44	218277.34	375876.13
302	98+63.53	21.85' RT	1179.21	218276.26	375877.36
303	98+63.21	20.16' RT	1178.89	218274.80	375878.27
304	98+61.80	20.16' RT	1179.04	218275.31	375879.59

PROJECT NO: 5540-02-71

HWY: STH 27

COUNTY: CRAWFORD

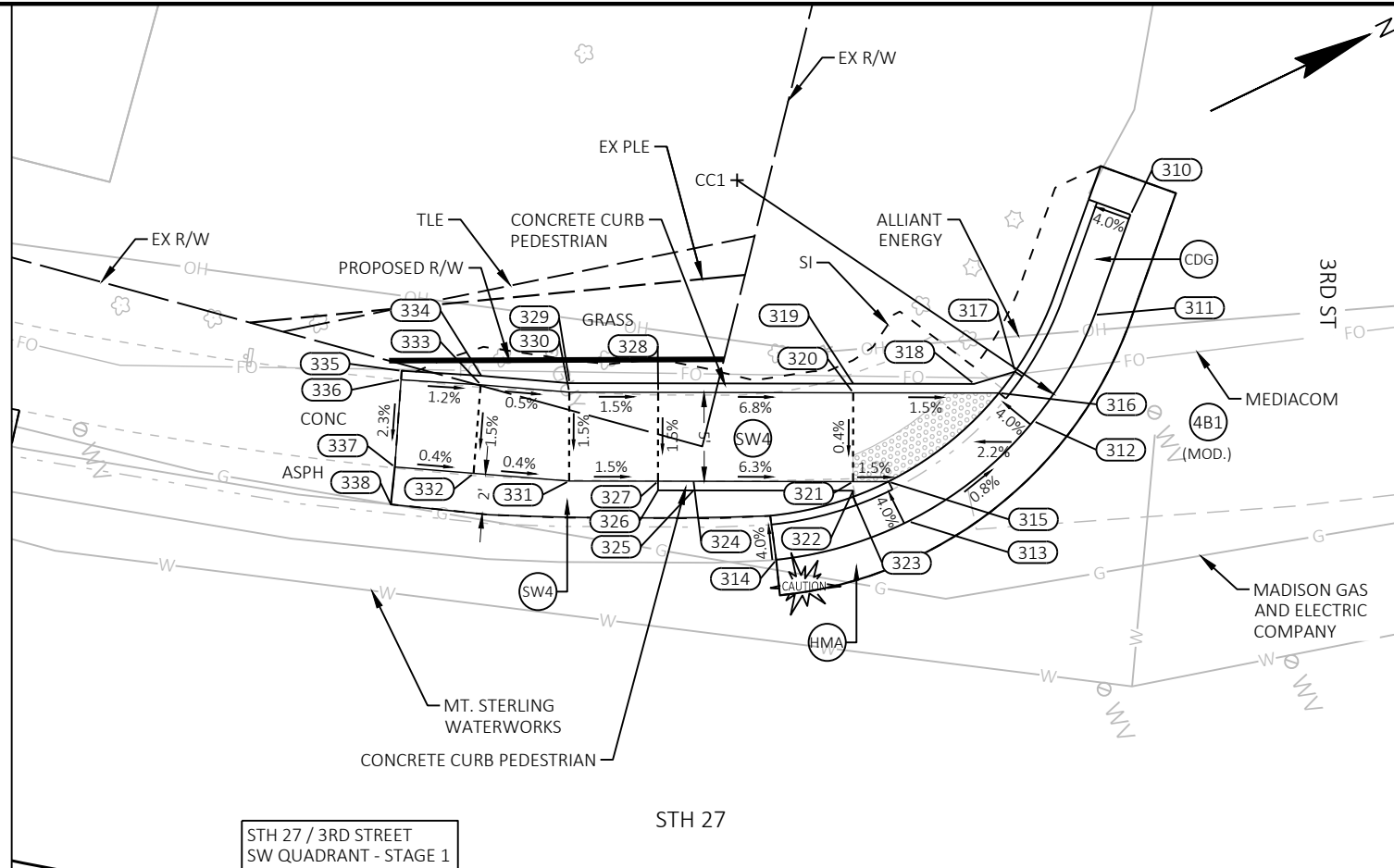
CURB RAMP DETAILS

SHEET

E

RADIUS POINT TABLE

POINT	STATION	OFFSET	RADIUS	COORDINATES
CC1	1191+53.89 - STH 27	43.1' LT	21.5'	Y = 218506.58 X = 375913.42



LEGEND

- (XXX) CURB RAMP TYPE
- (SW4) CONCRETE SIDEWALK 4-INCH OVER 4" BASE AGGREGATE DENSE 1 1/4-INCH
- (CGD) CONCRETE CURB & GUTTER 30-INCH TYPE D OVER 9.5" BASE AGGREGATE DENSE 1 1/4-INCH
- (HMA) 5 1/2" HMA PAVEMENT (1.75" UPPER LAYER, 3.75" LOWER LAYER) OVER 9.5" BASE AGGREGATE DENSE 1 1/4-INCH
- [Yellow Box] CURB RAMP DETECTABLE WARNING FIELD (YELLOW)

STH 27 / 3RD STREET SW QUADRANT - STAGE 1

RADIAL DWF FEATURES	
BACK OF CURB RADIUS	19'
LANDING LENGTH 'XR'	8.2'
RADIAL WARNING FIELD AREA	17.1 SF
RADIAL LONG CHORD	8'

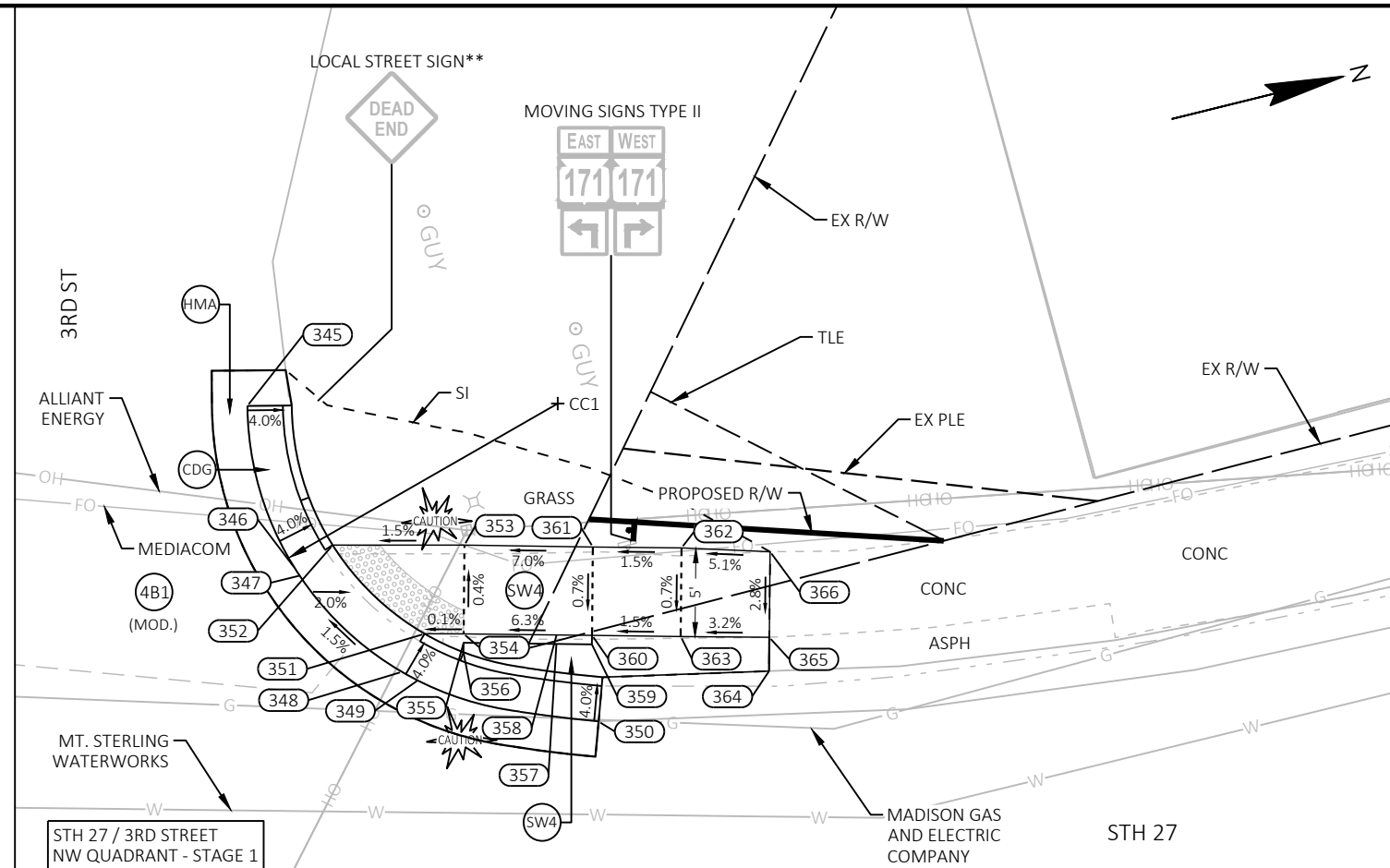
STH 27 & 3RD STREET - SW QUAD					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
310	1191+80.49	40.94' LT	1180.39	218525.69	375924.75
311	1191+77.72	35.42' LT	1180.60	218521.42	375928.97
312	1191+73.47	29.61' LT	1180.85	218515.74	375932.87
313	1191+65.48	24.10' LT	1180.92	218506.96	375934.91
314	1191+57.31	21.83' LT	1181.13	218499.31	375933.65
315	1191+64.23	26.33' LT	1180.84	218506.92	375932.41
316	1191+71.33	31.25' LT	1180.77	218514.68	375930.60
317	1191+72.39	32.42' LT	1181.21	218516.01	375929.91
318	1191+69.72	31.79' LT	1181.29	218513.63	375929.53
319	1191+61.94	31.83' LT	1181.39	218507.51	375926.58
320	1191+61.94	31.32' LT	1180.89	218507.29	375927.04
321	1191+62.00	26.32' LT	1180.87	218505.11	375931.54
322	1191+62.00	25.81' LT	1181.37	218504.89	375932.00
323	1191+62.02	25.46' LT	1181.41	218504.75	375932.33
324	1191+51.97	26.06' LT	1181.44	218497.03	375927.65
325	1191+52.00	25.55' LT	1181.94	218496.81	375928.11
326	1191+49.78	25.45' LT	1181.56	218495.00	375927.24
327	1191+49.74	25.96' LT	1181.56	218495.23	375926.78
328	1191+49.41	30.96' LT	1182.14	218497.40	375922.27
329	1191+43.70	31.10' LT	1182.21	218493.13	375919.65

STH 27 & 3RD STREET - SW QUAD					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
330	1191+43.72	30.59' LT	1181.71	218492.89	375920.10
331	1191+44.18	25.61' LT	1181.64	218490.72	375924.61
332	1191+38.17	25.54' LT	1181.66	218486.06	375921.89
333	1191+38.03	30.53' LT	1181.73	218488.58	375917.57
334	1191+38.01	31.04' LT	1182.23	218488.83	375917.13
335	1191+32.89	30.79' LT	1181.79	218484.91	375914.94
336	1191+32.92	30.30' LT	1181.79	218484.67	375915.37
337	1191+33.23	25.38' LT	1181.68	218482.22	375919.65
338	1191+33.28	23.26' LT	1181.74	218481.11	375921.45

NOTES:
 1. CONTRACTOR TO VERIFY ELEVATIONS, GRADES, SLOPES, LENGTHS AND MATCH POINTS PRIOR TO CURB RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST ELEVATIONS TO FIT FIELD CONDITIONS.
 3. DASHED LINES SHOWN FOR SIDEWALK ARE FOR INFORMATION ONLY AND DO NOT INDICATE JOINT LOCATIONS.
 4. SEE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS.
 5. ALL STATION AND OFFSET INFORMATION REFERENCE THE STH 27 R/L.
 6. SEE TRAFFIC CONTROL - STAGE 1 PEDESTRIAN ACCOMMODATION FOR ADDITIONAL INFORMATION.

RADIUS POINT TABLE

POINT	STATION	OFFSET	RADIUS	COORDINATES
CC1	1192+25.60 - STH 27	39.6' LT	17.5'	Y = 218561.47 X = 375936.98



LEGEND

- (XXX) CURB RAMP TYPE
- (SW4) CONCRETE SIDEWALK 4-INCH OVER 4" BASE AGGREGATE DENSE 1 1/4-INCH
- (CGD) CONCRETE CURB & GUTTER 30-INCH TYPE D OVER 9.5" BASE AGGREGATE DENSE 1 1/4-INCH
- (HMA) 5 1/2" HMA PAVEMENT (1.75" UPPER LAYER, 3.75" LOWER LAYER) OVER 9.5" BASE AGGREGATE DENSE 1 1/4-INCH
- [Pattern] CURB RAMP DETECTABLE WARNING FIELD (YELLOW)

RADIAL DWF FEATURES	
BACK OF CURB RADIUS	15'
LANDING LENGTH 'XR'	7.4'
RADIAL WARNING FIELD AREA	15.9 SF
RADIAL LONG CHORD	7.3'

STH 27 & 3RD STREET - NW QUAD					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
345	1192+04.78	39.38' LT	1180.65	218544.42	375932.94
346	1192+08.32	30.31' LT	1180.82	218544.84	375942.50
347	1192+08.62	29.93' LT	1180.83	218544.99	375942.94
348	1192+15.21	24.61' LT	1180.95	218549.31	375949.58
349	1192+16.18	24.13' LT	1180.96	218550.05	375950.26
350	1192+27.36	21.58' LT	1181.11	218559.40	375954.95
351	1192+16.62	26.75' LT	1180.87	218551.05	375947.80
352	1192+10.69	31.70' LT	1180.74	218547.21	375941.71
353	1192+19.21	31.74' LT	1180.85	218554.43	375943.49
354	1192+19.09	26.74' LT	1180.87	218553.19	375948.32
355	1192+19.08	26.23' LT	1181.37	218553.07	375948.82
356	1192+19.07	25.70' LT	1181.41	218552.94	375949.34
357	1192+24.92	26.05' LT	1181.69	218558.14	375950.13
358	1192+24.95	26.56' LT	1181.20	218558.27	375949.64
359	1192+27.15	25.94' LT	1181.33	218560.07	375950.64

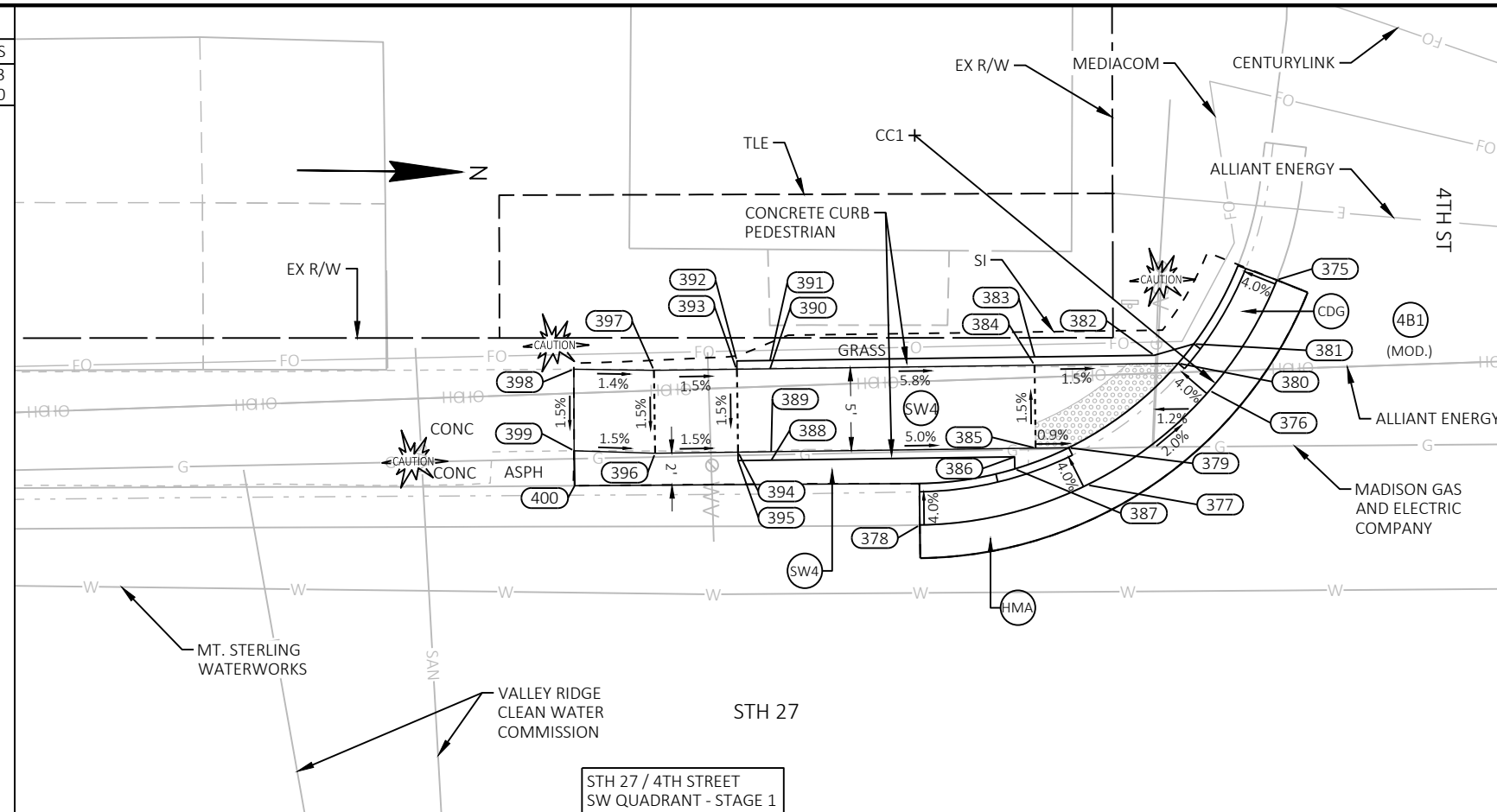
STH 27 & 3RD STREET - NW QUAD					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
360	1192+27.19	26.45' LT	1181.33	218560.20	375950.14
361	1192+27.49	31.43' LT	1181.36	218561.44	375945.32
362	1192+33.19	31.10' LT	1181.44	218566.28	375946.56
363	1192+32.76	26.11' LT	1181.40	218565.04	375951.40
364	1192+38.11	23.79' LT	1181.48	218569.42	375954.48
365	1192+38.32	25.68' LT	1181.56	218569.89	375952.64
366	1192+38.85	30.49' LT	1181.69	218571.08	375947.96

**NOTE: CONTACT VILLAGE OF MT STERLING TO HAVE THE MUNICIPALITY MOVE THIS SIGN PRIOR TO CONSTRUCTION.

- NOTES:
- CONTRACTOR TO VERIFY ELEVATIONS, GRADES, SLOPES, LENGTHS AND MATCH POINTS PRIOR TO CURB RAMP AND SIDEWALK CONSTRUCTION.
 - THE ENGINEER MAY ADJUST ELEVATIONS TO FIT FIELD CONDITIONS.
 - DASHED LINES SHOWN FOR SIDEWALK ARE FOR INFORMATION ONLY AND DO NOT INDICATE JOINT LOCATIONS.
 - SEE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS.
 - ALL STATION AND OFFSET INFORMATION REFERENCE THE STH 27 R/L.
 - SEE TRAFFIC CONTROL - STAGE 1 PEDESTRIAN ACCOMMODATION FOR ADDITIONAL INFORMATION.

RADIUS POINT TABLE

POINT	STATION	OFFSET	RADIUS	COORDINATES
CC1	1193+55.05 - STH 27	43.5' LT	23.5' R	Y = 218681.93 X = 375936.30



LEGEND

- (XXX) CURB RAMP TYPE
- (SW4) CONCRETE SIDEWALK 4-INCH OVER 4" BASE AGGREGATE DENSE 1 1/4-INCH
- (CGD) CONCRETE CURB & GUTTER 30-INCH TYPE D OVER 9.5" BASE AGGREGATE DENSE 1 1/4-INCH
- (HMA) 5 1/2" HMA PAVEMENT (1.75" UPPER LAYER, 3.75" LOWER LAYER) OVER 9.5" BASE AGGREGATE DENSE 1 1/4-INCH
- [Yellow Box] CURB RAMP DETECTABLE WARNING FIELD (YELLOW)

RADIAL DWF FEATURES	
BACK OF CURB RADIUS	21'
LANDING LENGTH 'XR'	8.7'
RADIAL WARNING FIELD AREA	17.8 SF
RADIAL LONG CHORD	8.4'

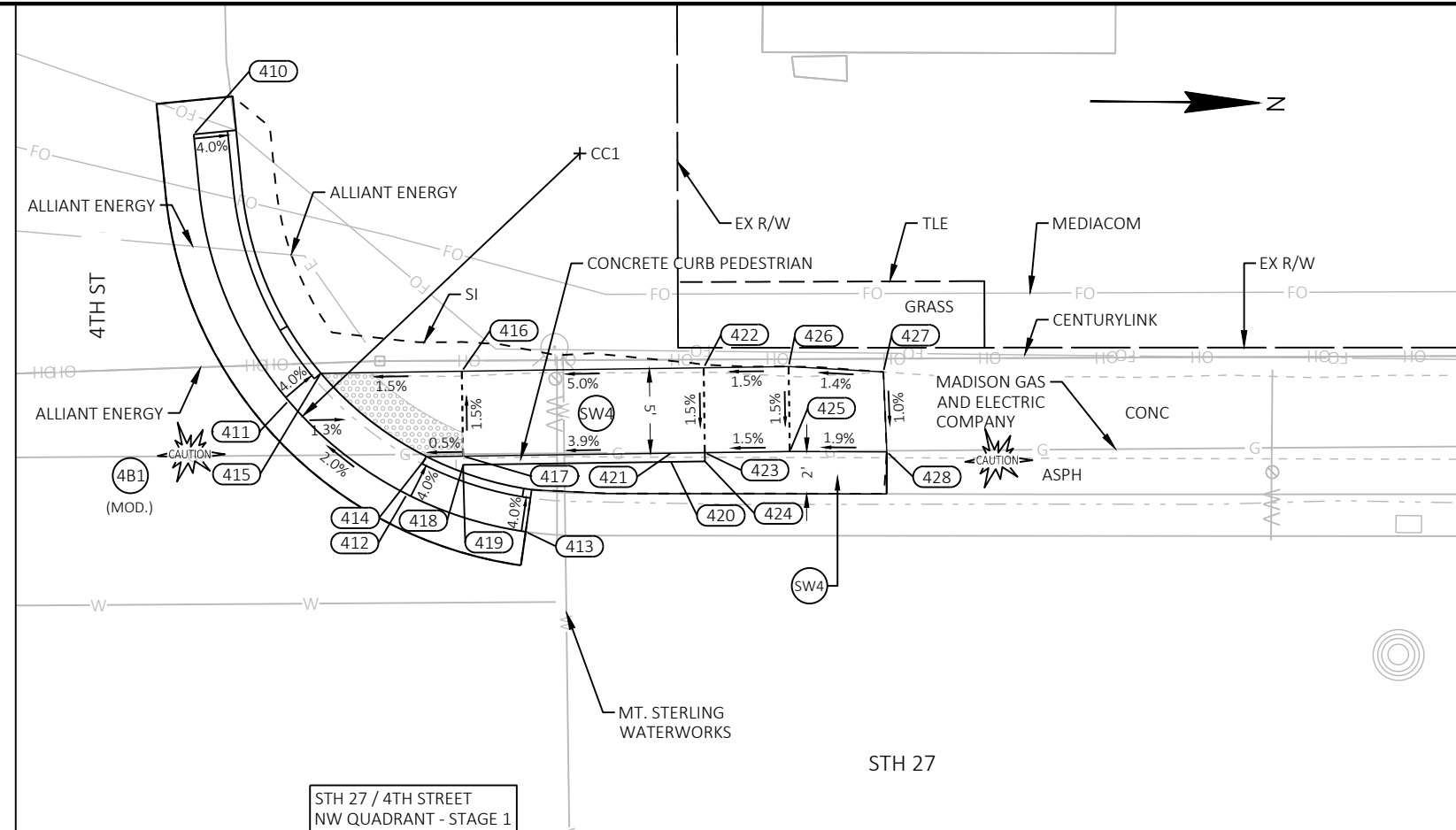
STH 27 & 4TH STREET - SW QUAD					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
375	1193+76.83	34.75' LT	1179.71	218703.83	375944.77
376	1193+72.77	28.10' LT	1180.07	218699.87	375951.48
377	1193+65.38	22.43' LT	1180.26	218692.56	375957.26
378	1193+55.29	20.04' LT	1180.63	218682.51	375959.79
379	1193+64.29	24.68' LT	1180.18	218691.43	375955.03
380	1193+70.89	29.74' LT	1179.99	218697.96	375949.86
381	1193+71.83	30.91' LT	1180.42	218698.89	375948.68
382	1193+69.45	30.23' LT	1180.51	218696.51	375949.40
383	1193+62.24	30.16' LT	1180.62	218689.30	375949.57
384	1193+62.23	29.65' LT	1180.12	218689.31	375950.08
385	1193+62.29	24.65' LT	1180.19	218689.43	375955.08
386	1193+61.02	24.13' LT	1180.76	218688.17	375955.62
387	1193+61.02	23.40' LT	1180.69	218688.19	375956.34
388	1193+46.36	23.98' LT	1181.49	218673.51	375955.98
389	1193+46.35	24.49' LT	1180.99	218673.50	375955.47
390	1193+46.30	29.49' LT	1181.05	218673.38	375950.47
391	1193+46.29	30.00' LT	1181.55	218673.37	375949.96
392	1193+44.30	29.97' LT	1181.16	218671.37	375950.02
393	1193+44.27	29.47' LT	1181.16	218671.35	375950.52
394	1193+44.35	24.47' LT	1181.09	218671.50	375955.52

STH 27 & 4TH STREET - SW QUAD					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
395	1193+44.36	23.96' LT	1181.09	218671.52	375956.03
396	1193+39.35	24.41' LT	1181.16	218666.50	375955.65
397	1193+39.30	29.41' LT	1181.24	218666.38	375950.65
398	1193+34.47	29.50' LT	1181.31	218661.55	375950.64
399	1193+34.48	24.58' LT	1181.24	218661.62	375955.55
400	1193+34.50	22.47' LT	1181.16	218661.68	375957.66

NOTES:
 1. CONTRACTOR TO VERIFY ELEVATIONS, GRADES, SLOPES, LENGTHS AND MATCH POINTS PRIOR TO CURB RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST ELEVATIONS TO FIT FIELD CONDITIONS.
 3. DASHED LINES SHOWN FOR SIDEWALK ARE FOR INFORMATION ONLY AND DO NOT INDICATE JOINT LOCATIONS.
 4. SEE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS.
 5. ALL STATION AND OFFSET INFORMATION REFERENCE THE STH 27 R/L.
 6. SEE TRAFFIC CONTROL - STAGE 1 PEDESTRIAN ACCOMMODATION FOR ADDITIONAL INFORMATION.

RADIUS POINT TABLE

POINT	STATION	OFFSET	RADIUS	COORDINATES
CC1	1194+21.23 - STH 27	42.6' LT	22.5' R	Y = 218748.11 X = 375936.29



LEGEND

- (XXX) CURB RAMP TYPE
- (SW4) CONCRETE SIDEWALK 4-INCH OVER 4" BASE AGGREGATE DENSE 1 1/4-INCH
- (CGD) CONCRETE CURB & GUTTER 30-INCH TYPE D OVER 9.5" BASE AGGREGATE DENSE 1 1/4-INCH
- (HMA) 5 1/2" HMA PAVEMENT (1.75" UPPER LAYER, 3.75" LOWER LAYER) OVER 9.5" BASE AGGREGATE DENSE 1 1/4-INCH
- (Yellow Box) CURB RAMP DETECTABLE WARNING FIELD (YELLOW)

STH 27 / 4TH STREET
NW QUADRANT - STAGE 1

RADIAL DWF FEATURES

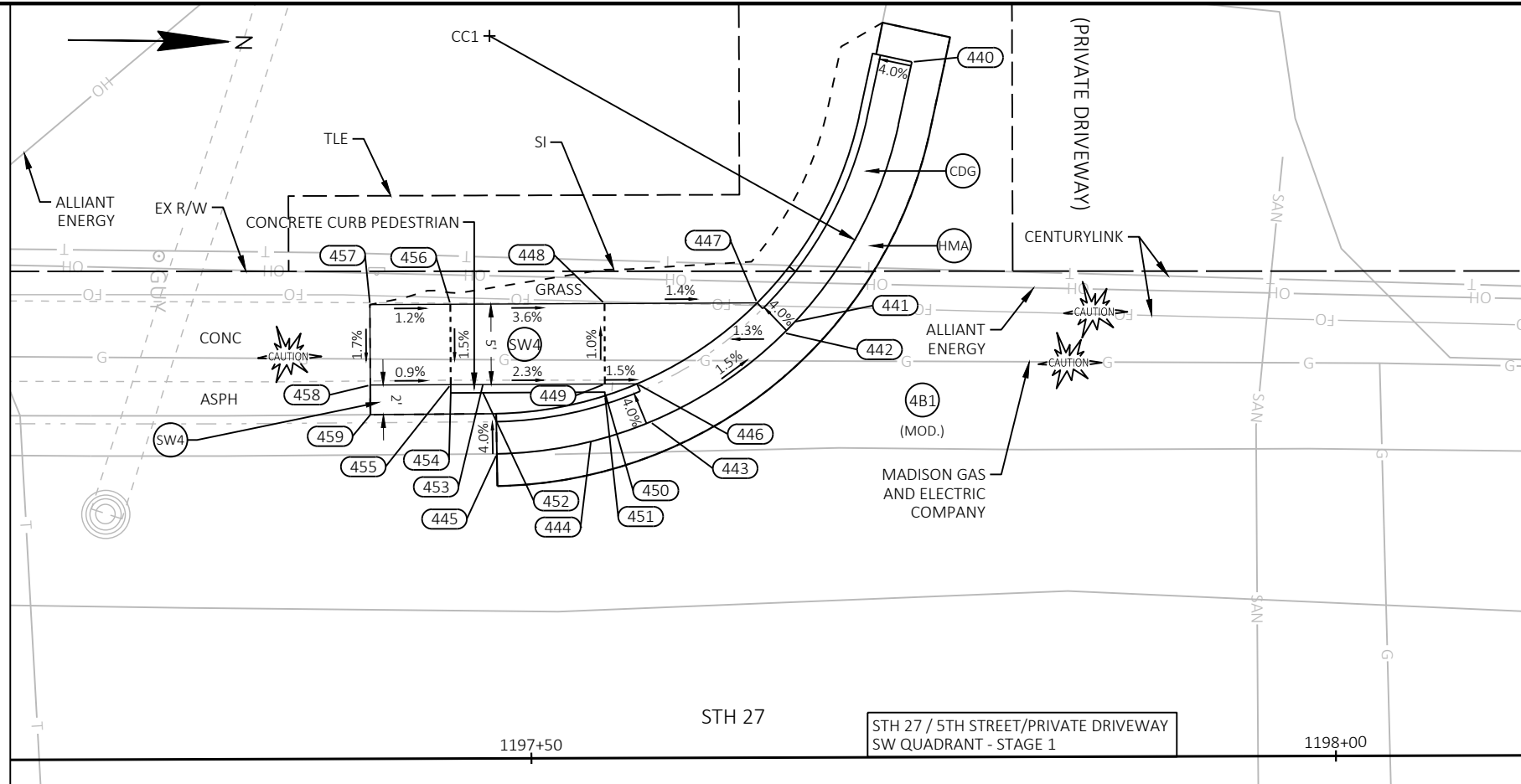
BACK OF CURB RADIUS	20'
LANDING LENGTH 'XR'	8.3'
RADIAL WARNING FIELD AREA	17.2 SF
RADIAL LONG CHORD	8'

STH 27 & 4TH STREET - NW QUAD

POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
410	1193+98.51	43.77' LT	1179.17	218725.37	375935.44
411	1194+04.04	28.08' LT	1180.06	218731.13	375951.05
412	1194+10.99	22.56' LT	1180.24	218738.16	375956.47
413	1194+17.97	20.33' LT	1180.39	218745.17	375958.59
414	1194+12.17	24.77' LT	1180.17	218739.31	375954.25
415	1194+05.95	29.69' LT	1179.98	218733.02	375949.41
416	1194+14.24	29.79' LT	1180.10	218741.31	375949.19
417	1194+14.30	24.79' LT	1180.18	218741.44	375954.19
418	1194+14.30	24.28' LT	1180.68	218741.45	375954.70
419	1194+14.31	23.83' LT	1180.71	218741.46	375955.15
420	1194+26.53	24.42' LT	1181.16	218753.68	375954.39
421	1194+26.53	24.93' LT	1180.66	218753.66	375953.88
422	1194+28.47	29.95' LT	1180.81	218755.53	375948.83
423	1194+28.53	24.95' LT	1180.74	218755.66	375953.82
424	1194+28.53	24.44' LT	1180.74	218755.67	375954.33
425	1194+33.53	25.01' LT	1180.81	218760.66	375953.70
426	1194+33.47	30.01' LT	1180.89	218760.53	375948.70
427	1194+39.04	29.67' LT	1180.97	218766.11	375948.95
428	1194+39.23	24.97' LT	1180.92	218766.36	375953.65

- NOTES:**
1. CONTRACTOR TO VERIFY ELEVATIONS, GRADES, SLOPES, LENGTHS AND MATCH POINTS PRIOR TO CURB RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST ELEVATIONS TO FIT FIELD CONDITIONS.
 3. DASHED LINES SHOWN FOR SIDEWALK ARE FOR INFORMATION ONLY AND DO NOT INDICATE JOINT LOCATIONS.
 4. SEE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS.
 5. ALL STATION AND OFFSET INFORMATION REFERENCE THE STH 27 R/L.
 6. SEE TRAFFIC CONTROL - STAGE 1 PEDESTRIAN ACCOMMODATION FOR ADDITIONAL INFORMATION.

RADIUS POINT TABLE				
POINT	STATION	OFFSET	RADIUS	COORDINATES
CC1	1197+47.53 - STH 27	44.9' LT	26.0' R	Y = 219074.35 X = 375929.26

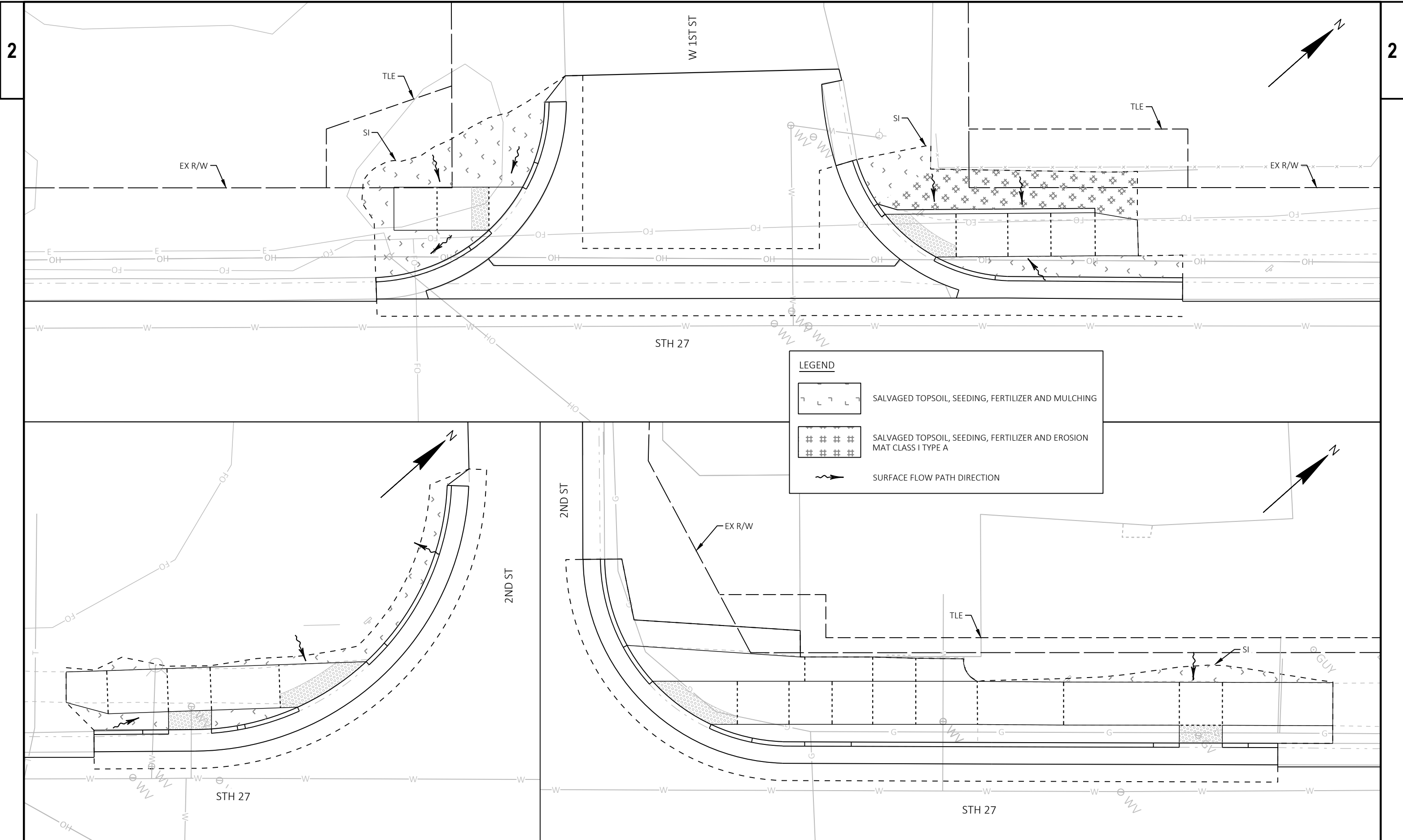


LEGEND	
(XXX)	CURB RAMP TYPE
(SW4)	CONCRETE SIDEWALK 4-INCH OVER 4" BASE AGGREGATE DENSE 1 1/4-INCH
(CGD)	CONCRETE CURB & GUTTER 30-INCH TYPE D OVER 9.5" BASE AGGREGATE DENSE 1 1/4-INCH
(HMA)	5 1/2" HMA PAVEMENT (1.75" UPPER LAYER, 3.75" LOWER LAYER) OVER 9.5" BASE AGGREGATE DENSE 1 1/4-INCH
(Yellow Box)	CURB RAMP DETECTABLE WARNING FIELD (YELLOW)

STH 27 & 5TH STREET - SW QUAD					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
440	1197+73.77	43.22' LT	1182.77	219100.61	375930.58
441	1197+66.45	27.09' LT	1183.48	219093.52	375946.82
442	1197+65.78	26.41' LT	1183.49	219092.86	375947.51
443	1197+57.44	20.89' LT	1183.64	219084.60	375953.14
444	1197+53.76	19.68' LT	1183.70	219080.93	375954.41
445	1197+47.91	18.93' LT	1183.63	219075.10	375955.25
446	1197+56.63	23.26' LT	1183.57	219083.76	375950.79
447	1197+64.11	28.27' LT	1183.41	219091.16	375945.67
448	1197+54.62	28.26' LT	1183.55	219081.68	375945.82
449	1197+54.63	23.26' LT	1183.60	219081.76	375950.82
450	1197+54.63	22.75' LT	1184.10	219081.76	375951.33
451	1197+54.64	22.53' LT	1184.10	219081.77	375951.55
452	1197+47.06	22.74' LT	1184.27	219074.19	375951.45
453	1197+47.06	23.25' LT	1183.77	219074.18	375950.94
454	1197+45.05	22.73' LT	1183.82	219072.19	375951.48

STH 27 & 5TH STREET - SW QUAD					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
455	1197+45.05	23.24' LT	1183.82	219072.18	375950.97
456	1197+45.05	28.24' LT	1183.89	219072.10	375945.97
457	1197+40.05	28.24' LT	1183.95	219067.10	375946.05
458	1197+40.05	23.24' LT	1183.87	219067.18	375951.05
459	1197+40.06	21.39' LT	1183.87	219067.22	375952.90

- NOTES:
1. CONTRACTOR TO VERIFY ELEVATIONS, GRADES, SLOPES, LENGTHS AND MATCH POINTS PRIOR TO CURB RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST ELEVATIONS TO FIT FIELD CONDITIONS.
 3. DASHED LINES SHOWN FOR SIDEWALK ARE FOR INFORMATION ONLY AND DO NOT INDICATE JOINT LOCATIONS.
 4. SEE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS.
 5. ALL STATION AND OFFSET INFORMATION REFERENCE THE STH 27 R/L.
 6. SEE TRAFFIC CONTROL - STAGE 1 PEDESTRIAN ACCOMMODATION FOR ADDITIONAL INFORMATION.



PROJECT NO: 5540-02-71

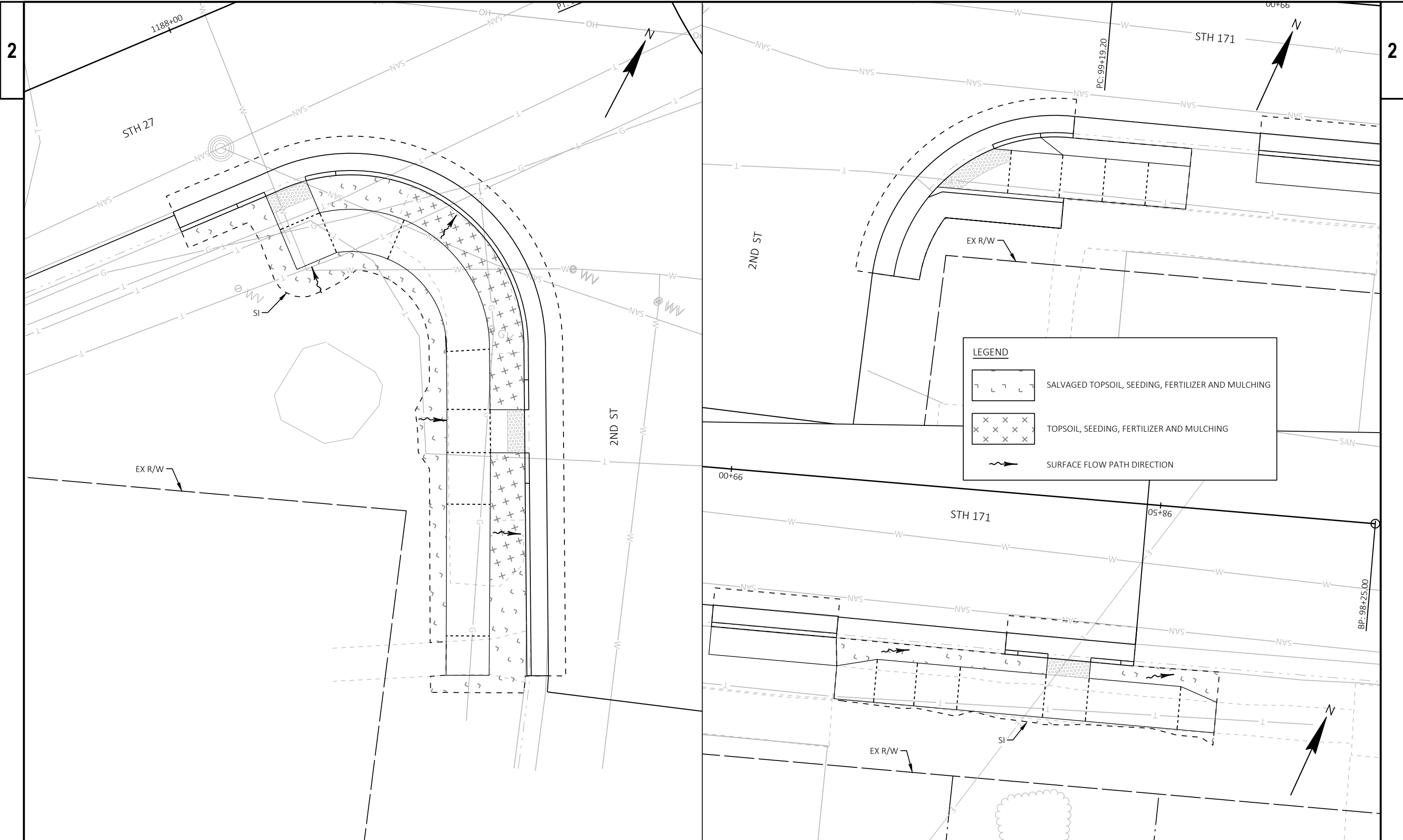
HWY: STH 27

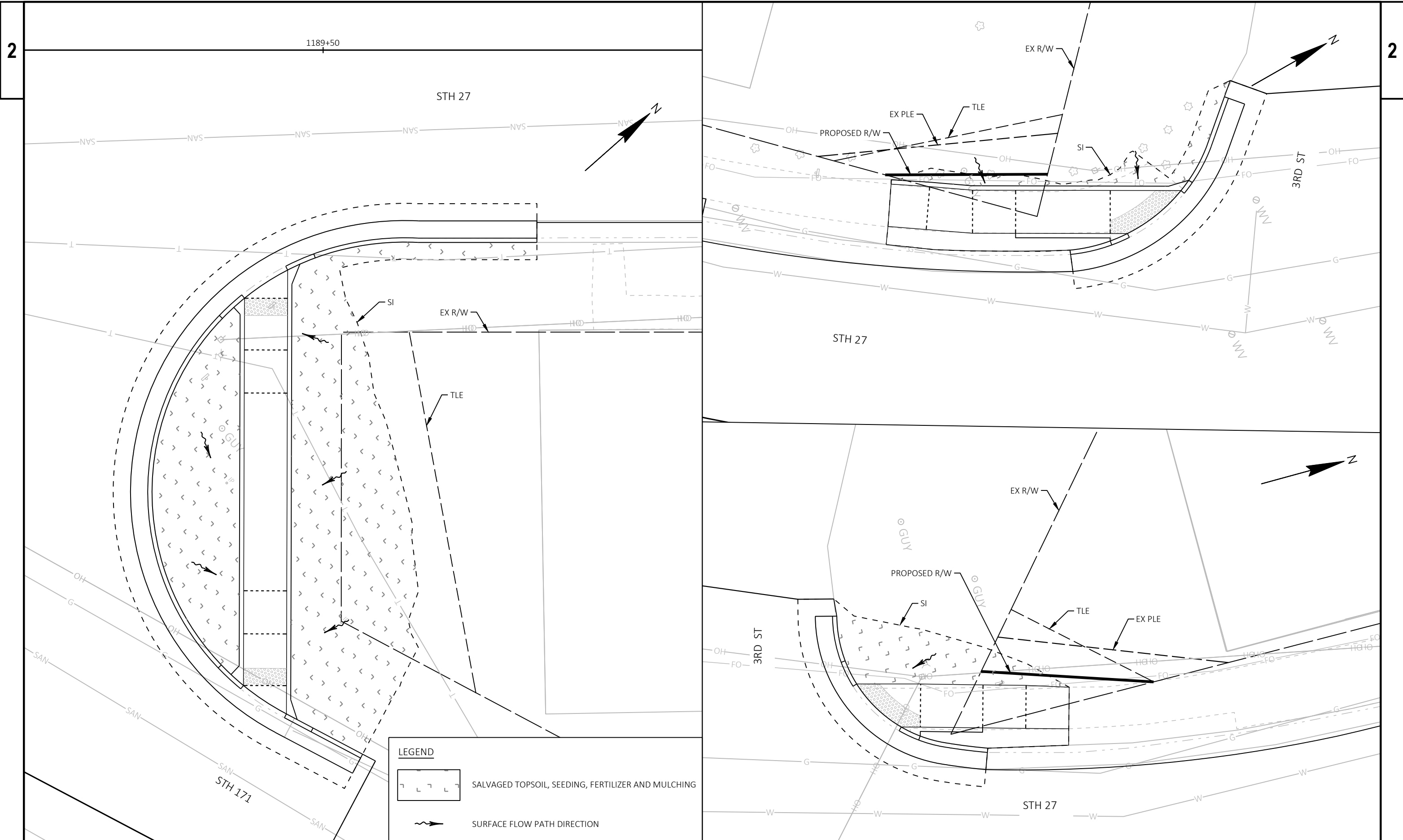
COUNTY: CRAWFORD

EROSION CONTROL



SHEET

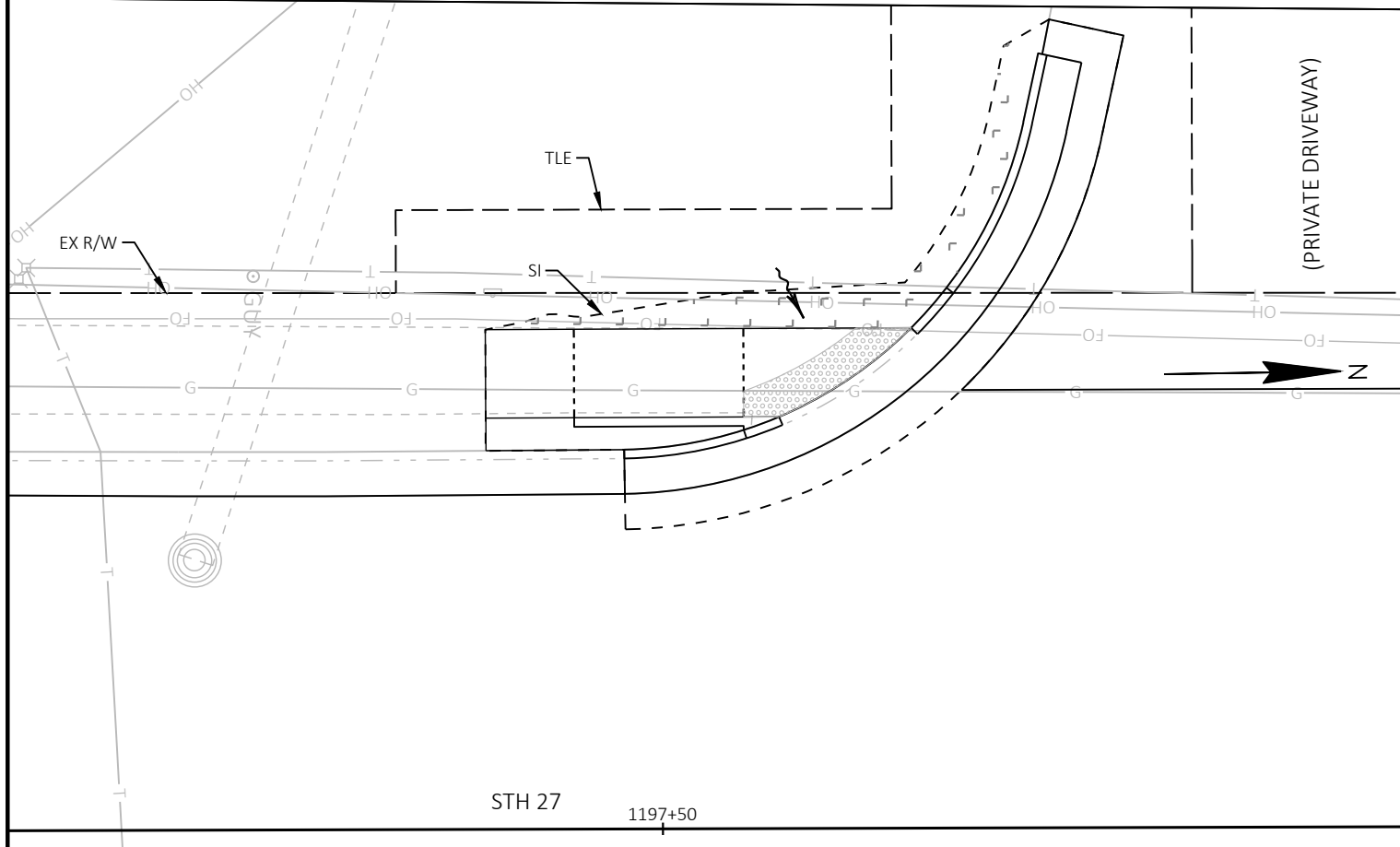
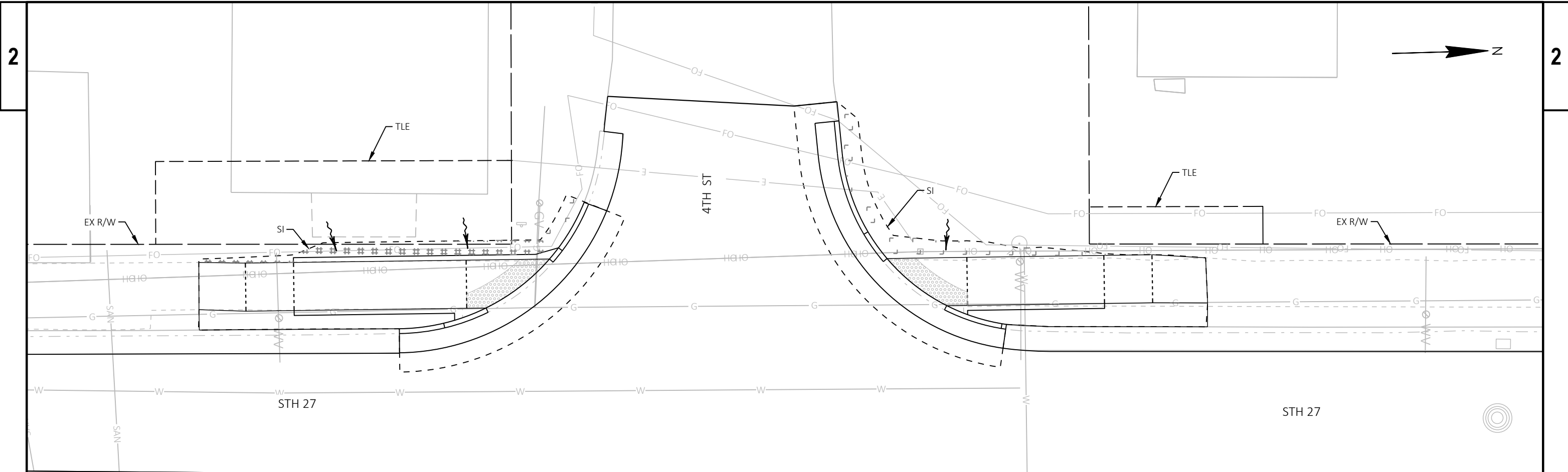
E








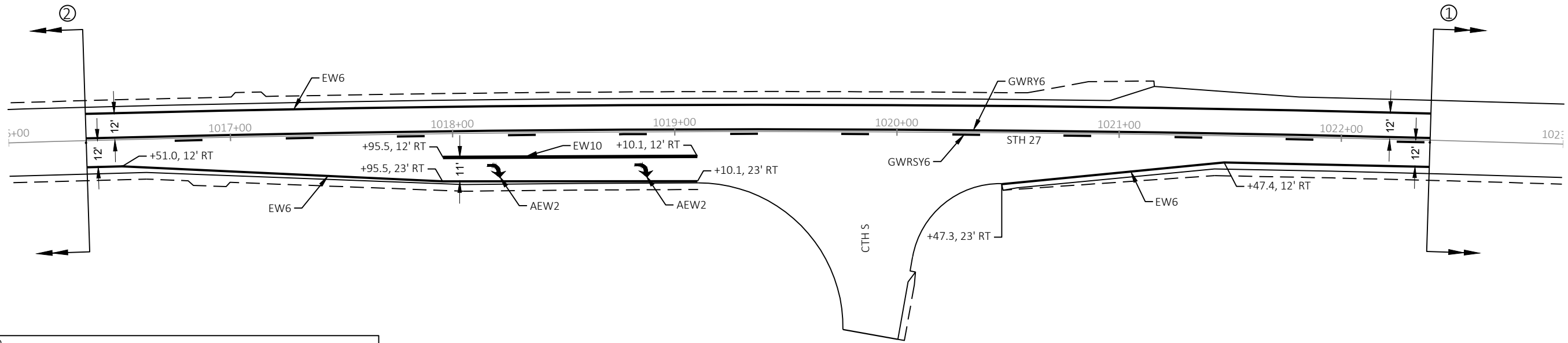
LEGEND

-  SALVAGED TOPSOIL, SEEDING, FERTILIZER AND MULCHING
-  SURFACE FLOW PATH DIRECTION



LEGEND

-  SALVAGED TOPSOIL, SEEDING, FERTILIZER AND MULCHING
-  SALVAGED TOPSOIL, SEEDING, FERTILIZER AND EROSION MAT CLASS I TYPE A
-  SURFACE FLOW PATH DIRECTION



LEGEND

AEW2	MARKING ARROW EPOXY TYPE 2 (WHITE)
EY6	MARKING LINE EPOXY 6-INCH (YELLOW)
EW6	MARKING LINE EPOXY 6-INCH (WHITE)
ESY6	MARKING LINE EPOXY 6-INCH (SKIP, YELLOW) (12.5' SEG, 37.5' GAP)
EW10	MARKING LINE EPOXY 10-INCH (WHITE)
GWR6	MARKING LINE GROOVED WET REFLECTIVE EPOXY 6-INCH (YELLOW)
GWR6SY6	MARKING LINE GROOVED WET REFLECTIVE EPOXY 6-INCH (SKIP, YELLOW) (12.5' SEG, 37.5' GAP)
SLE18	MARKING STOP LINE EPOXY 18-INCH (WHITE)

PAVEMENT MARKING NOTES

PROVIDE TEMPORARY MARKING LINE PAINT 6-INCH (DOUBLE YELLOW) CENTERLINE MARKINGS ON LOWER HMA LAYERS.

PROVIDE MARKING LINE SAME DAY EPOXY 6-INCH (YELLOW) CENTERLINE MARKINGS ON UPPER HMA LAYER AT LOCATIONS WITHOUT CENTERLINE RUMBLESTRIPS.

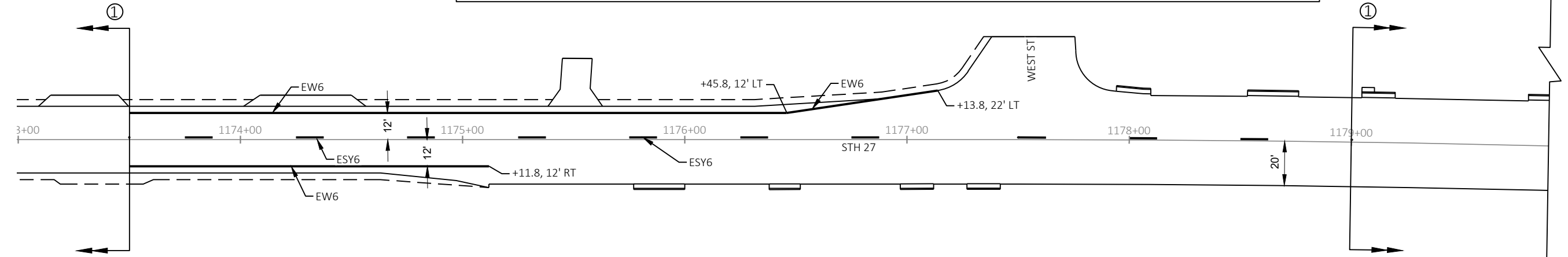
PROVIDE TEMPORARY MARKING LINE PAINT 6-INCH (YELLOW) CENTERLINE MARKINGS ON UPPER HMA LAYER AT LOCATIONS WITH CENTERLINE RUMBLESTRIPS IN EXACT LOCATION OF FINAL CENTERLINE MARKINGS PRIOR TO RUMBLESTRIP PLACEMENT. MARKING LINE GROOVED WET REFLECTIVE EPOXY 6-INCH (YELLOW) CENTERLINE MARKINGS TO BE PLACED AFTER CENTERLINE RUMBLESTRIPS HAVE BEEN PLACED.

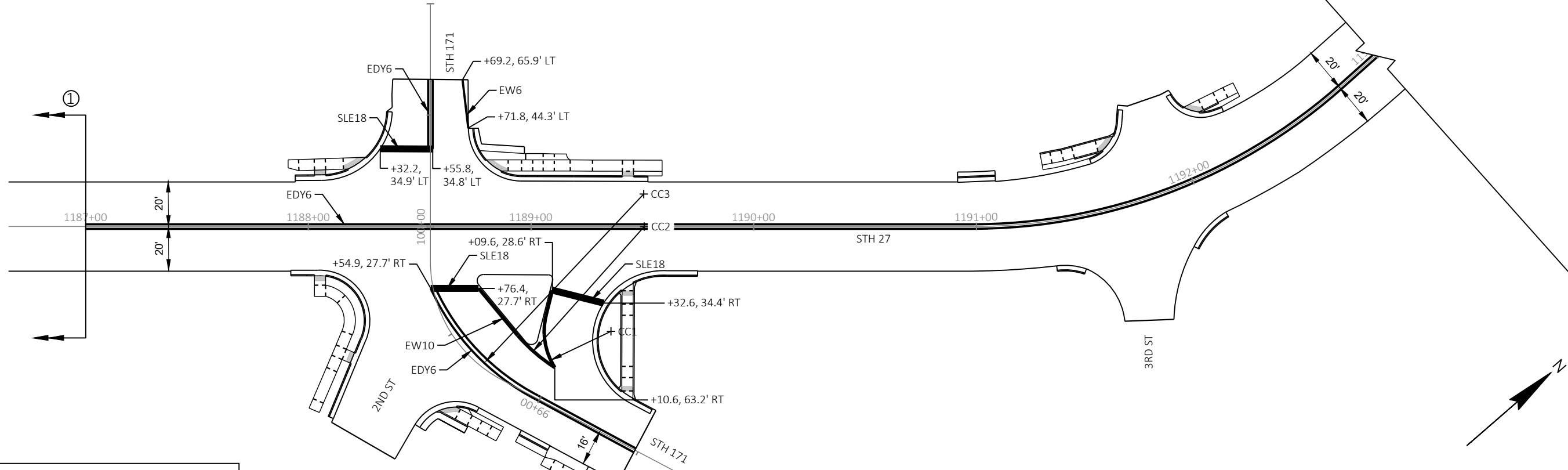
① FOLLOW SDD "PERMANENT LONGITUDINAL PAVEMENT MARKINGS" FOR PLACEMENT OF LONGITUDINAL CENTERLINE & EDGELINE MARKINGS.

② FOLLOW SDD "PERMANENT LONGITUDINAL PAVEMENT MARKINGS" FOR PLACEMENT OF LONGITUDINAL CENTERLINE & EDGELINE MARKINGS TO PROJECT BEGINNING.

FOLLOW SDD "PAVEMENT MARKING ARROWS" FOR ARROW MARKING SIZES.

FOLLOW SDD "PAVEMENT MARKING (TURN LANES)" FOR PLACEMENT OF TURN LANE AND ARROW MARKINGS.





LEGEND

EW6	MARKING LINE EPOXY 6-INCH (WHITE)
EDY6	MARKING LINE EPOXY 6-INCH (DOUBLE YELLOW)
EW10	MARKING LINE EPOXY 10-INCH (WHITE)
SLE18	MARKING STOP LINE EPOXY 18-INCH (WHITE)

RADIUS POINT TABLE

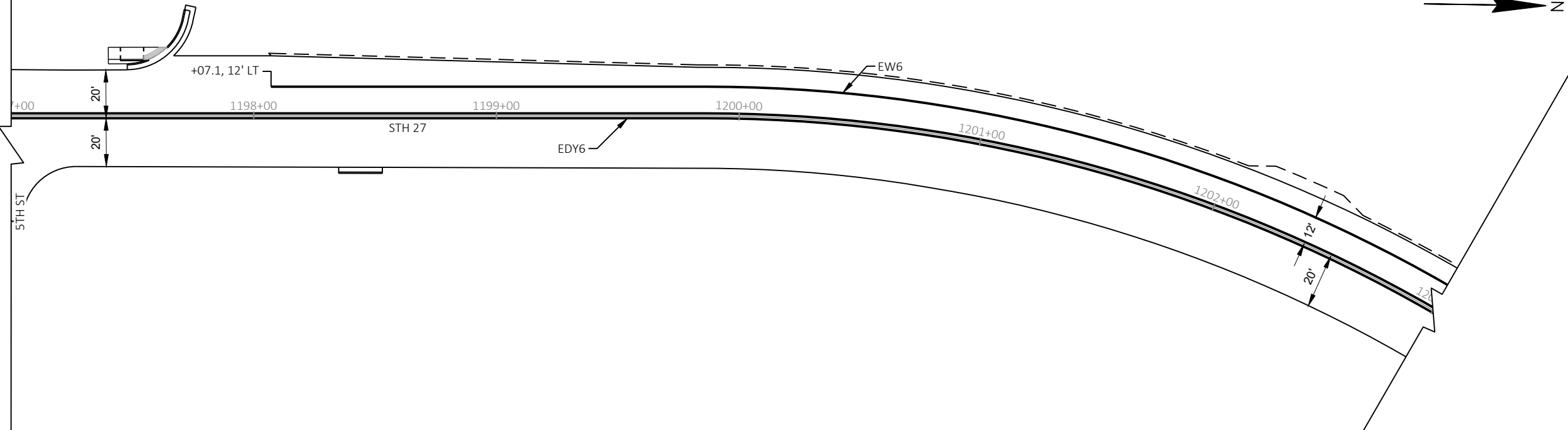
POINT	STATION	OFFSET	RADIUS	COORDINATES
CC1	1189+35.88 - STH 27	47.3' RT	30.0' R	Y = 218286.81 X = 375849.38
CC2	1189+50.79 - STH 27	0.1' LT	75.0' R	Y = 218329.10 X = 375823.38
CC3	1189+50.58 - STH 27	14.61' LT	103.0' R	Y = 218338.51 X = 375812.21

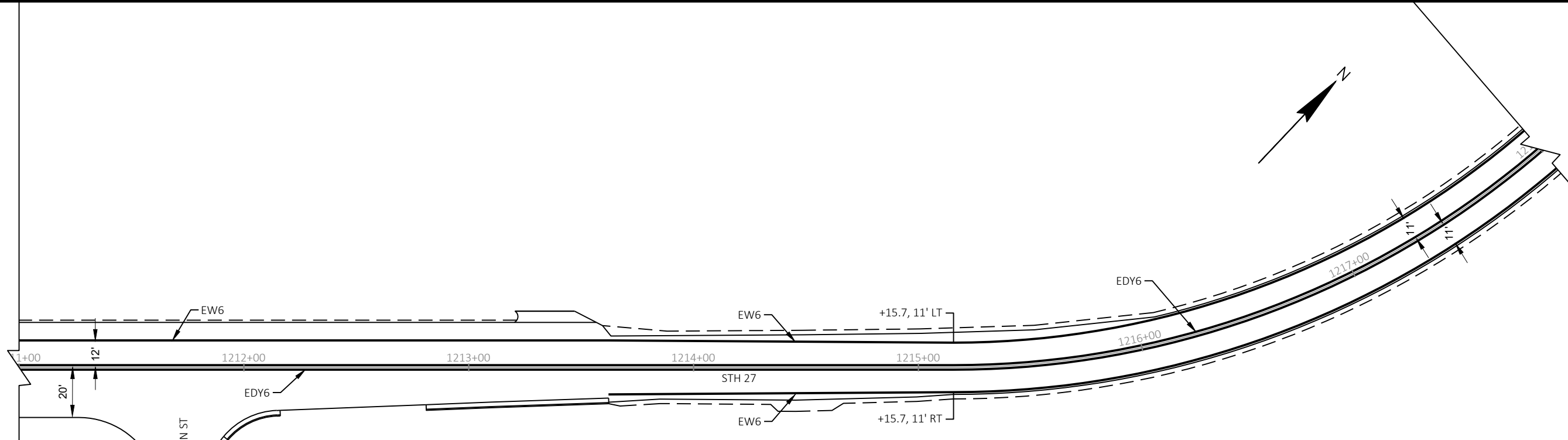
PAVEMENT MARKING NOTES

PROVIDE TEMPORARY MARKING LINE PAINT 6-INCH (DOUBLE YELLOW) CENTERLINE MARKINGS ON LOWER HMA LAYERS.

① FOLLOW SDD "PERMANENT LONGITUDINAL PAVEMENT MARKINGS" FOR PLACEMENT OF LONGITUDINAL CENTERLINE & EDGELINE MARKINGS.

FOLLOW SDD "STOP LINE AND CROSSWALK PAVEMENT MARKING" FOR PLACEMENT OF STOP LINES.





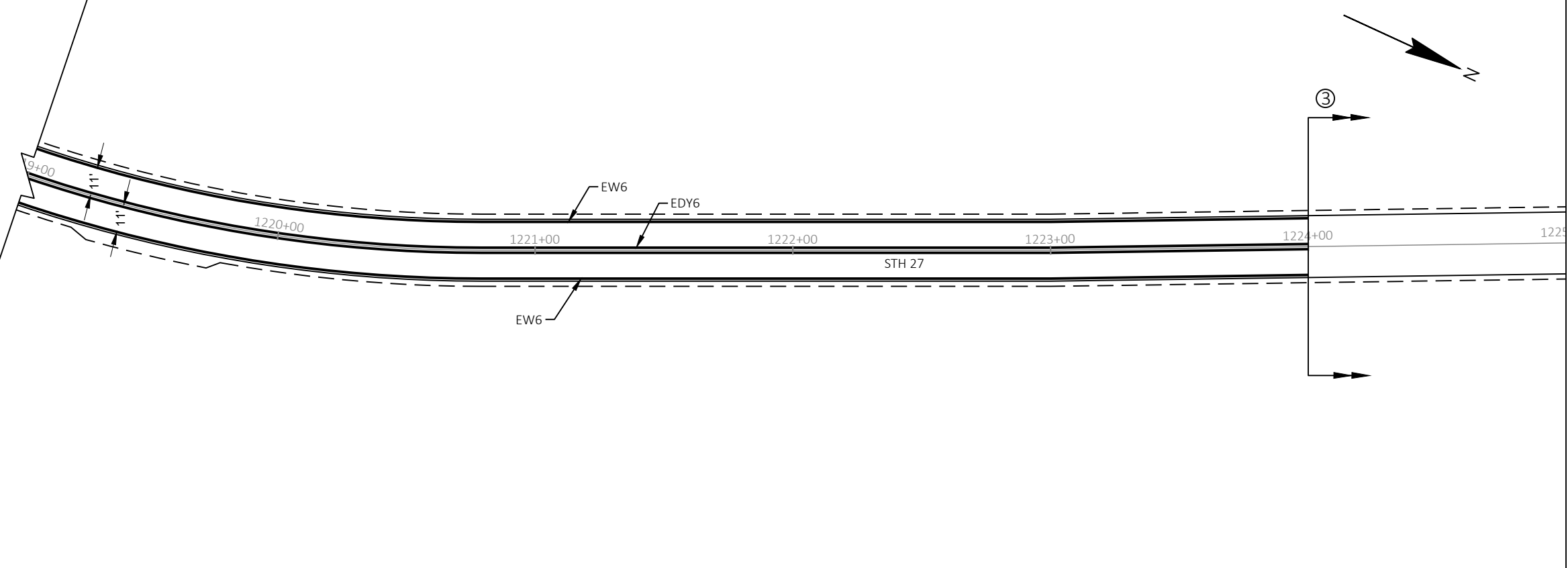
LEGEND

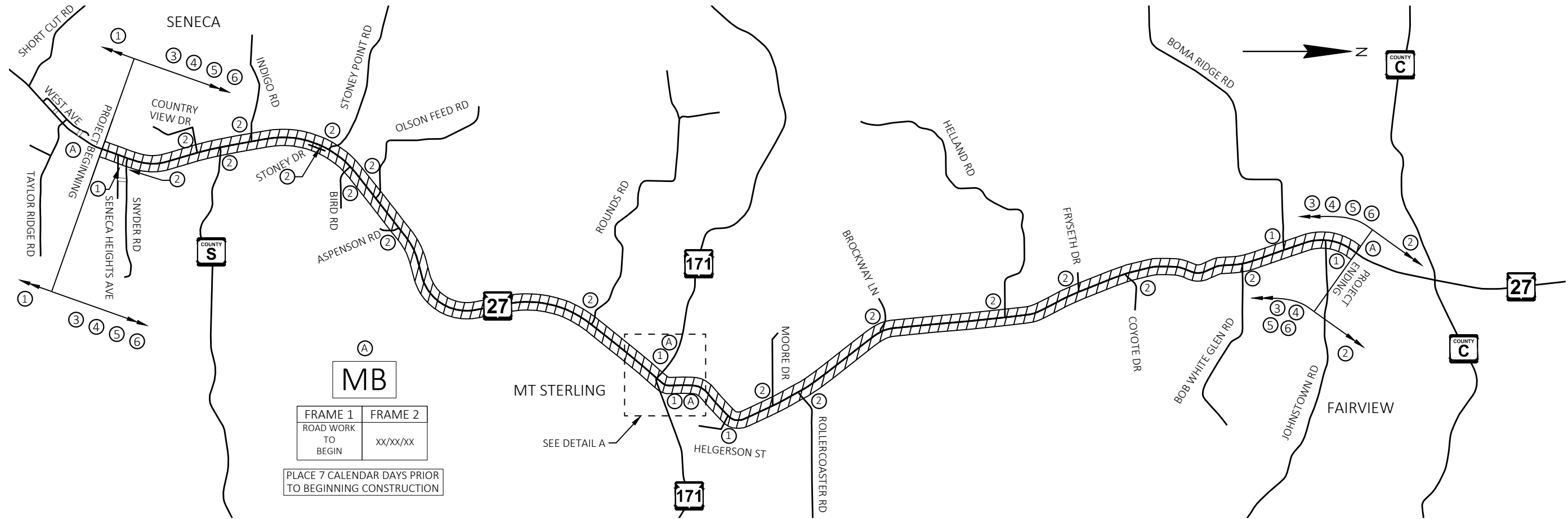
EY6	MARKING LINE EPOXY 6-INCH (YELLOW)
EDY6	MARKING LINE EPOXY 6-INCH (DOUBLE YELLOW)
EW6	MARKING LINE EPOXY 6-INCH (WHITE)

PAVEMENT MARKING NOTES

PROVIDE TEMPORARY MARKING LINE PAINT 6-INCH (DOUBLE YELLOW) CENTERLINE MARKINGS ON LOWER HMA LAYERS.

③ FOLLOW SDD "PERMANENT LONGITUDINAL PAVEMENT MARKINGS" FOR PLACEMENT OF LONGITUDINAL CENTERLINE & EDGELINE MARKINGS TO PROJECT END.





A	
MB	
FRAME 1	FRAME 2
ROAD WORK TO BEGIN	xx/xx/xx
PLACE 7 CALENDAR DAYS PRIOR TO BEGINNING CONSTRUCTION	

TRAFFIC CONTROL GENERAL NOTES:
DRAWING IS NOT TO SCALE.

ALL TRAFFIC CONTROL SIGNS AND DEVICES AND THEIR LOCATIONS SHALL BE IN CONFORMANCE WITH THE WISCONSIN MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD), THE PLANS, STANDARD SPECIFICATIONS AND APPLICABLE STANDARD DETAIL DRAWINGS.

ADJUST THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

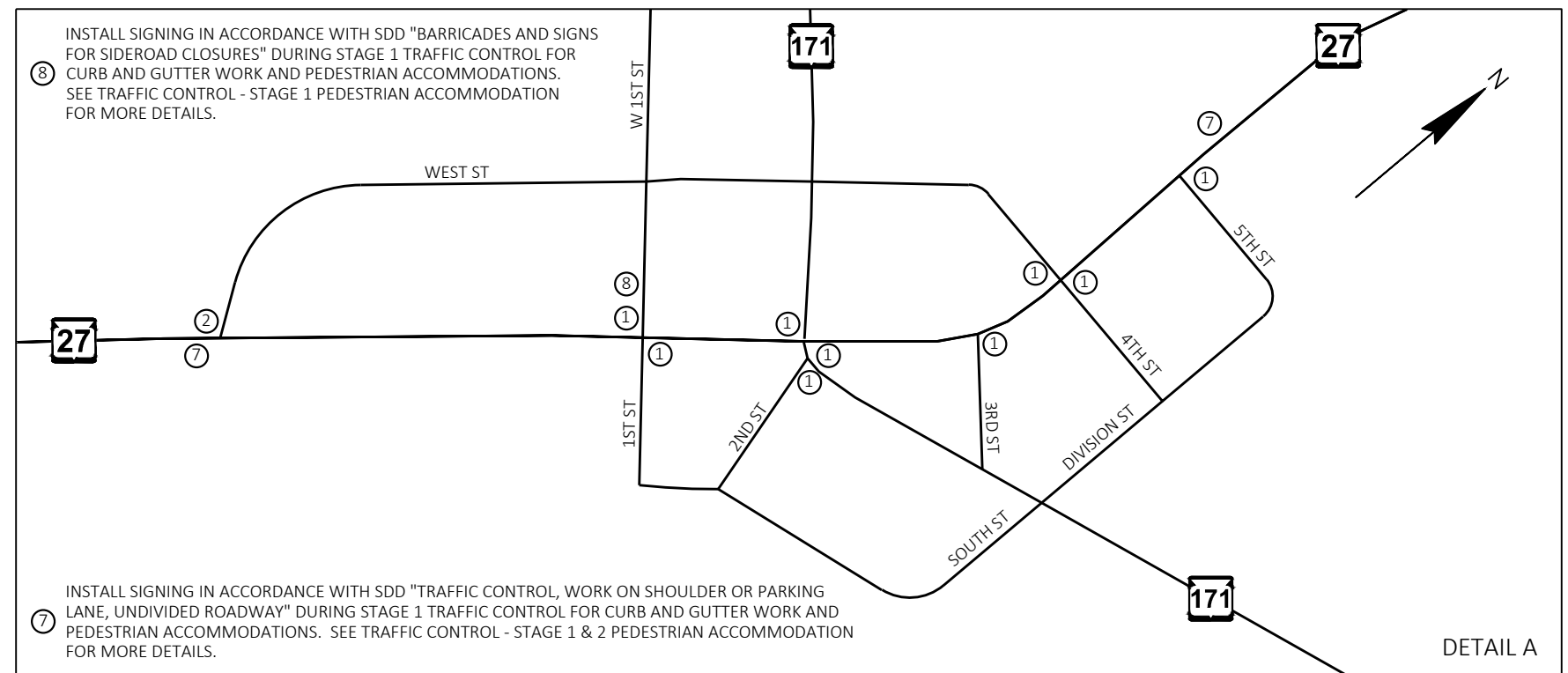
ALL W SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

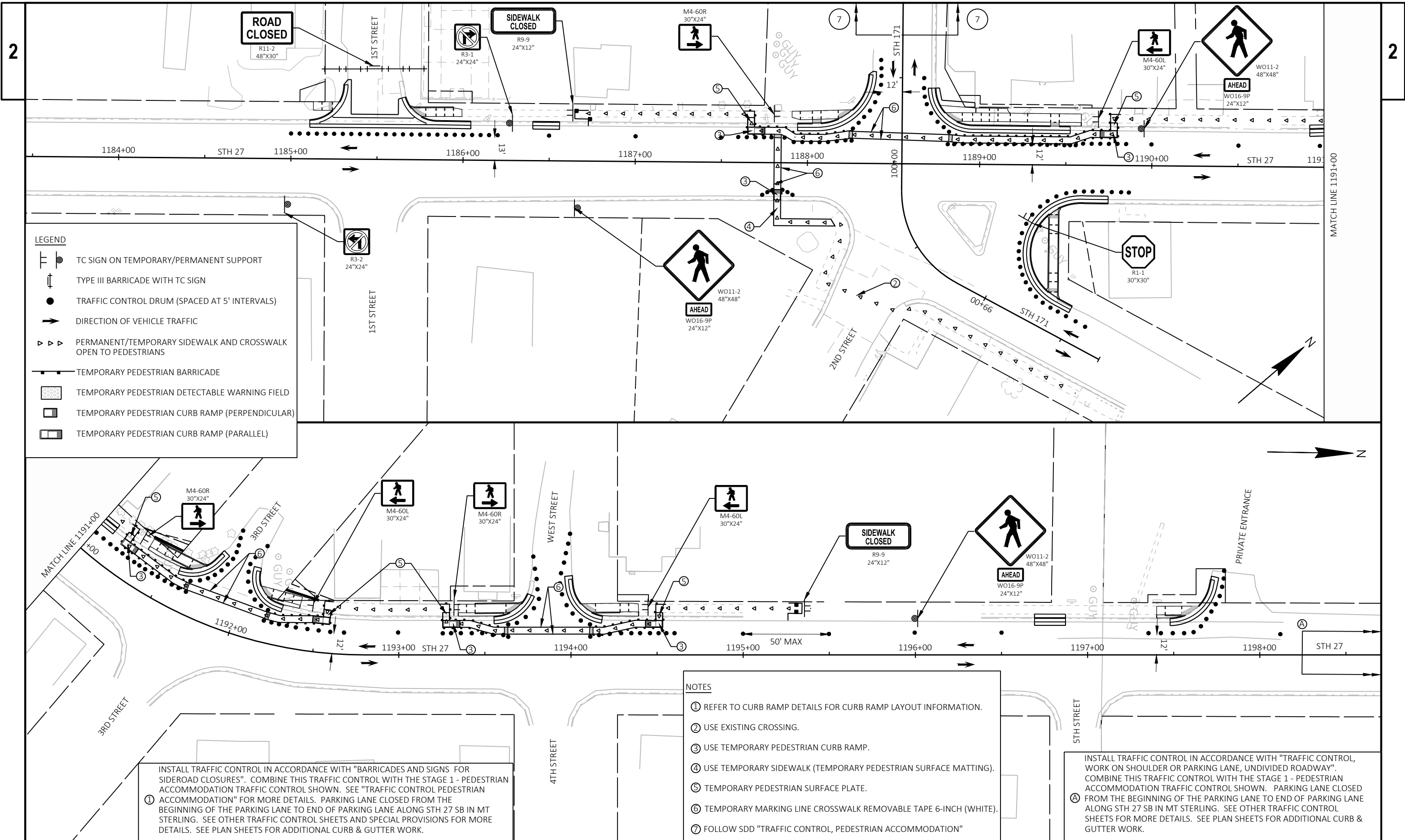
"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

"MO" SIGNS ARE THE SAME AS "M" SIGNS EXCEPT THE BACK GROUND IS ORANGE.

- ① INSTALL ADVANCED WARNING SIGNS ON STH 27 PRIOR TO THE PROJECT BEGINNING WORK ZONE AND ON ALL SIDEROADS LOCATED WITHIN THE PROJECT LIMITS WITH SPEED LIMITS 40 M.P.H. OR LESS, IN ACCORDANCE WITH SDD "TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M.P.H. OR LESS, TWO WAY UNDIVIDED ROAD OPEN TO TRAFFIC".
- ② INSTALL ADVANCED WARNING SIGNS ON STH 27 PRIOR TO THE PROJECT ENDING WORK ZONE AND ON ALL SIDEROADS LOCATED WITHIN THE PROJECT LIMITS WITH SPEED LIMITS 45 M.P.H. OR GREATER, IN ACCORDANCE WITH SDD "TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M.P.H. OR GREATER, TWO WAY UNDIVIDED ROAD OPEN TO TRAFFIC".
- ③ UTILIZE FLAGGING OPERATIONS IN ACCORDANCE WITH SDD "TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION" AND CONFORMING TO THE STANDARD SPECIFICATIONS WHEN CLOSING A LANE FOR CONSTRUCTION OPERATIONS.
- ④ INSTALL TRAFFIC CONTROL SIGNS IN ACCORDANCE WITH SDD "TRAFFIC CONTROL FOR DROP-OFF SIGNING" DURING WORK OPERATIONS THAT CREATE DROP-OFFS OR HAZARDS AS DEFINED BY THE STANDARD SPECIFICATIONS.
- ⑤ INSTALL SIGNING IN ACCORDANCE WITH SDD "TRAFFIC CONTROL, SIGNING ON ROADWAYS WITH MILLED SURFACES" DURING THE MILLING OPERATIONS.
- ⑥ INSTALL SIGNING IN ACCORDANCE WITH SDD "TRAFFIC CONTROL, SIGNING ON ROADWAYS WITH LOOSE GRAVEL" WHEN PAVEMENT IS REMOVED AND TRAFFIC NEEDS TO RUN ON ANY REMAINING GRAVEL SURFACE.

LEGEND:





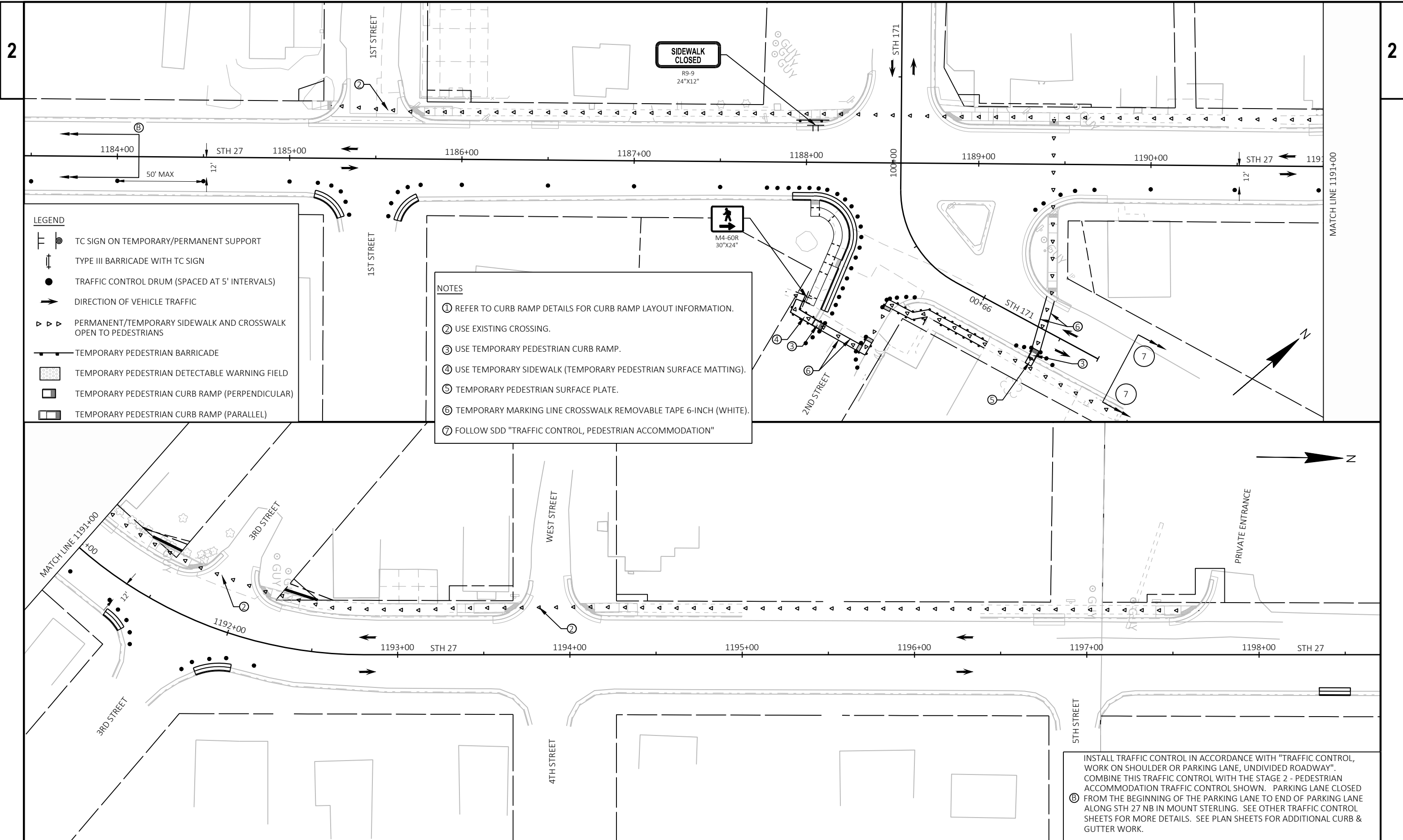
LEGEND

- TC SIGN ON TEMPORARY/PERMANENT SUPPORT
- TYPE III BARRICADE WITH TC SIGN
- TRAFFIC CONTROL DRUM (SPACED AT 5' INTERVALS)
- DIRECTION OF VEHICLE TRAFFIC
- PERMANENT/TEMPORARY SIDEWALK AND CROSSWALK OPEN TO PEDESTRIANS
- TEMPORARY PEDESTRIAN BARRICADE
- TEMPORARY PEDESTRIAN DETECTABLE WARNING FIELD
- TEMPORARY PEDESTRIAN CURB RAMP (PERPENDICULAR)
- TEMPORARY PEDESTRIAN CURB RAMP (PARALLEL)

- NOTES**
- ① REFER TO CURB RAMP DETAILS FOR CURB RAMP LAYOUT INFORMATION.
 - ② USE EXISTING CROSSING.
 - ③ USE TEMPORARY PEDESTRIAN CURB RAMP.
 - ④ USE TEMPORARY SIDEWALK (TEMPORARY PEDESTRIAN SURFACE MATTING).
 - ⑤ TEMPORARY PEDESTRIAN SURFACE PLATE.
 - ⑥ TEMPORARY MARKING LINE CROSSWALK REMOVABLE TAPE 6-INCH (WHITE).
 - ⑦ FOLLOW SDD "TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION"

INSTALL TRAFFIC CONTROL IN ACCORDANCE WITH "BARRICADES AND SIGNS FOR SIDEROAD CLOSURES". COMBINE THIS TRAFFIC CONTROL WITH THE STAGE 1 - PEDESTRIAN ACCOMMODATION TRAFFIC CONTROL SHOWN. SEE "TRAFFIC CONTROL PEDESTRIAN ACCOMMODATION" FOR MORE DETAILS. PARKING LANE CLOSED FROM THE BEGINNING OF THE PARKING LANE TO END OF PARKING LANE ALONG STH 27 SB IN MT STERLING. SEE OTHER TRAFFIC CONTROL SHEETS AND SPECIAL PROVISIONS FOR MORE DETAILS. SEE PLAN SHEETS FOR ADDITIONAL CURB & GUTTER WORK.

INSTALL TRAFFIC CONTROL IN ACCORDANCE WITH "TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY". COMBINE THIS TRAFFIC CONTROL WITH THE STAGE 1 - PEDESTRIAN ACCOMMODATION TRAFFIC CONTROL SHOWN. PARKING LANE CLOSED FROM THE BEGINNING OF THE PARKING LANE TO END OF PARKING LANE ALONG STH 27 SB IN MT STERLING. SEE OTHER TRAFFIC CONTROL SHEETS FOR MORE DETAILS. SEE PLAN SHEETS FOR ADDITIONAL CURB & GUTTER WORK.



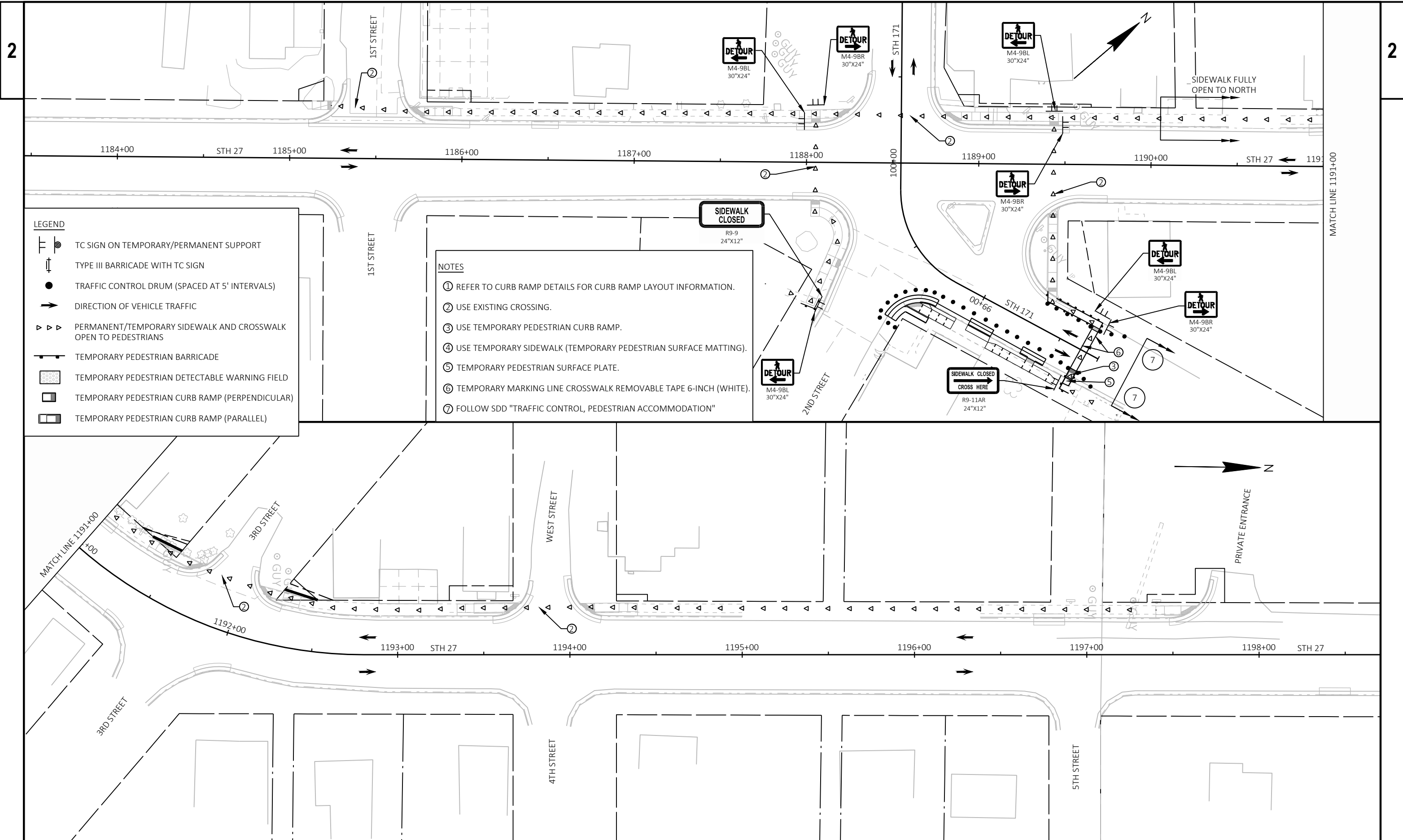
LEGEND

- TC SIGN ON TEMPORARY/PERMANENT SUPPORT
- TYPE III BARRICADE WITH TC SIGN
- TRAFFIC CONTROL DRUM (SPACED AT 5' INTERVALS)
- DIRECTION OF VEHICLE TRAFFIC
- PERMANENT/TEMPORARY SIDEWALK AND CROSSWALK OPEN TO PEDESTRIANS
- TEMPORARY PEDESTRIAN BARRICADE
- TEMPORARY PEDESTRIAN DETECTABLE WARNING FIELD
- TEMPORARY PEDESTRIAN CURB RAMP (PERPENDICULAR)
- TEMPORARY PEDESTRIAN CURB RAMP (PARALLEL)

NOTES

- ① REFER TO CURB RAMP DETAILS FOR CURB RAMP LAYOUT INFORMATION.
- ② USE EXISTING CROSSING.
- ③ USE TEMPORARY PEDESTRIAN CURB RAMP.
- ④ USE TEMPORARY SIDEWALK (TEMPORARY PEDESTRIAN SURFACE MATTING).
- ⑤ TEMPORARY PEDESTRIAN SURFACE PLATE.
- ⑥ TEMPORARY MARKING LINE CROSSWALK REMOVABLE TAPE 6-INCH (WHITE).
- ⑦ FOLLOW SDD "TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION"

INSTALL TRAFFIC CONTROL IN ACCORDANCE WITH "TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY". COMBINE THIS TRAFFIC CONTROL WITH THE STAGE 2 - PEDESTRIAN ACCOMMODATION TRAFFIC CONTROL SHOWN. PARKING LANE CLOSED FROM THE BEGINNING OF THE PARKING LANE TO END OF PARKING LANE ALONG STH 27 NB IN MOUNT STERLING. SEE OTHER TRAFFIC CONTROL SHEETS FOR MORE DETAILS. SEE PLAN SHEETS FOR ADDITIONAL CURB & GUTTER WORK.



- NOTES**
- ① REFER TO CURB RAMP DETAILS FOR CURB RAMP LAYOUT INFORMATION.
 - ② USE EXISTING CROSSING.
 - ③ USE TEMPORARY PEDESTRIAN CURB RAMP.
 - ④ USE TEMPORARY SIDEWALK (TEMPORARY PEDESTRIAN SURFACE MATTING).
 - ⑤ TEMPORARY PEDESTRIAN SURFACE PLATE.
 - ⑥ TEMPORARY MARKING LINE CROSSWALK REMOVABLE TAPE 6-INCH (WHITE).
 - ⑦ FOLLOW SDD "TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION"

- LEGEND**
- TC SIGN ON TEMPORARY/PERMANENT SUPPORT
 - TYPE III BARRICADE WITH TC SIGN
 - TRAFFIC CONTROL DRUM (SPACED AT 5' INTERVALS)
 - DIRECTION OF VEHICLE TRAFFIC
 - PERMANENT/TEMPORARY SIDEWALK AND CROSSWALK OPEN TO PEDESTRIANS
 - TEMPORARY PEDESTRIAN BARRICADE
 - TEMPORARY PEDESTRIAN DETECTABLE WARNING FIELD
 - TEMPORARY PEDESTRIAN CURB RAMP (PERPENDICULAR)
 - TEMPORARY PEDESTRIAN CURB RAMP (PARALLEL)

ELEVATIONS AT TEMPORARY CURB RAMP LOCATIONS

STAGE 1 PEDESTRIAN ACCOMMODATIONS					
RAMP TYPE	STATION	LOCATION	TOP OF CURB ELEVATION	PAVEMENT SURFACE ELEVATION	TEMPORARY RAMP LENGTH
PARALLEL	1187+75	18.4' LT	1181.68	1181.36	<6'
PERPENDICULAR	1187+83	15.6' RT	1181.84	1181.46	<6'
PARALLEL	1189+70	18.2' LT	1181.85	1181.62	<6'
PARALELL	1191+28	18.5' LT	1181.77	14177.28	<6'
PARALLEL	1192+48	18.8' LT	1181.51	1181.14	<6'
PARALLEL	1193+36	18' LT	1181.17	1180.82	<6'
PARALLEL	1194+43	18' LT	1180.96	1180.60	<6'

STAGE 2 PEDESTRIAN ACCOMMODATIONS					
RAMP TYPE	STATION	LOCATION	TOP OF CURB ELEVATION	PAVEMENT SURFACE ELEVATION	TEMPORARY RAMP LENGTH
PERPENDICULAR	98+59 (STH 171)	12.3' LT	1179.13	1178.96	<6'
PERPENDICULAR	99+45 (STH 171)	71.7' LT	1181.22	1180.81	<6'

STAGE 3 PEDESTRIAN ACCOMMODATIONS					
RAMP TYPE	STATION	LOCATION	TOP OF CURB ELEVATION	PAVEMENT SURFACE ELEVATION	TEMPORARY RAMP LENGTH
PERPENDICULAR	98+35 (STH 171)	12.2' LT	1178.12	1178.06	<6'

FIELD VERIFY ELEVATIONS AT FINAL TEMPORARY RAMP LOCATIONS

Estimate Of Quantities

5540-02-71

Line	Item	Item Description	Unit	Total	Qty
0002	201.0105	Clearing	STA	5.000	5.000
0004	201.0205	Grubbing	STA	5.000	5.000
0006	203.0100	Removing Small Pipe Culverts	EACH	14.000	14.000
0008	204.0110	Removing Asphaltic Surface	SY	1,185.000	1,185.000
0010	204.0115	Removing Asphaltic Surface Butt Joints	SY	910.000	910.000
0012	204.0120	Removing Asphaltic Surface Milling	SY	147,666.000	147,666.000
0014	204.0140	Removing Gutter	LF	58.000	58.000
0016	204.0150	Removing Curb & Gutter	LF	1,032.000	1,032.000
0018	204.0155	Removing Concrete Sidewalk	SY	248.000	248.000
0020	204.9060.S	Removing (item description) 01. Apron Endwall 24-Inch	EACH	4.000	4.000
0022	211.0101	Prepare Foundation for Asphaltic Paving (project) 01. 5540-02-71	EACH	1.000	1.000
0024	213.0100	Finishing Roadway (project) 01. 5540-02-71	EACH	1.000	1.000
0026	305.0110	Base Aggregate Dense 3/4-Inch	TON	1,212.000	1,212.000
0028	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	513.000	513.000
0030	416.0610	Drilled Tie Bars	EACH	124.000	124.000
0032	455.0605	Tack Coat	GAL	17,522.000	17,522.000
0034	460.0105.S	HMA Percent Within Limits (PWL) Test Strip Volumetrics	EACH	2.000	2.000
0036	460.0110.S	HMA Percent Within Limits (PWL) Test Strip Density	EACH	1.000	1.000
0038	460.2005	Incentive Density PWL HMA Pavement	DOL	11,167.000	11,167.000
0040	460.2007	Incentive Density HMA Pavement Longitudinal Joints	DOL	8,880.400	8,880.400
0042	460.2010	Incentive Air Voids HMA Pavement	DOL	14,565.000	14,565.000
0044	460.5223	HMA Pavement 3 LT 58-28 S	TON	19,268.000	19,268.000
0046	460.5224	HMA Pavement 4 LT 58-28 S	TON	14,564.000	14,564.000
0048	465.0105	Asphaltic Surface	TON	339.000	339.000
0050	465.0120	Asphaltic Surface Driveways and Field Entrances	TON	147.000	147.000
0052	465.0560	Asphaltic Rumble Strips, Centerline	LF	14,143.000	14,143.000
0054	520.8700	Cleaning Culvert Pipes	EACH	2.000	2.000
0056	520.9700.S	Culvert Pipe Liners (size) 01. Culvert Pipe Liners 18-Inch	LF	56.000	56.000
0058	520.9700.S	Culvert Pipe Liners (size) 02. Culvert Pipe Liners 24-Inch	LF	534.000	534.000
0060	520.9750.S	Cleaning Culvert Pipes for Liner Verification	EACH	6.000	6.000
0062	521.1018	Apron Endwalls for Culvert Pipe Steel 18-Inch	EACH	10.000	10.000
0064	521.1024	Apron Endwalls for Culvert Pipe Steel 24-Inch	EACH	20.000	20.000
0066	521.1030	Apron Endwalls for Culvert Pipe Steel 30-Inch	EACH	1.000	1.000
0068	521.3118	Culvert Pipe Corrugated Steel 18-Inch	LF	4.000	4.000
0070	521.3124	Culvert Pipe Corrugated Steel 24-Inch	LF	4.000	4.000
0072	521.3130	Culvert Pipe Corrugated Steel 30-Inch	LF	6.000	6.000
0074	524.0624	Apron Endwalls for Culvert Pipe Salvaged 24-Inch	EACH	6.000	6.000
0076	601.0411	Concrete Curb & Gutter 30-Inch Type D	LF	1,032.000	1,032.000
0078	601.0600	Concrete Curb Pedestrian	LF	233.000	233.000
0080	602.0405	Concrete Sidewalk 4-Inch	SF	2,789.000	2,789.000
0082	602.0415	Concrete Sidewalk 6-Inch	SF	156.000	156.000
0084	602.0505	Curb Ramp Detectable Warning Field Yellow	SF	81.000	81.000
0086	602.0605	Curb Ramp Detectable Warning Field Radial Yellow	SF	139.000	139.000
0088	606.0200	Riprap Medium	CY	36.000	36.000
0090	611.8110	Adjusting Manhole Covers	EACH	16.000	16.000
0092	614.0400	Adjusting Steel Plate Beam Guard	LF	150.000	150.000
0094	614.0950	Replacing Guardrail Posts and Blocks	EACH	5.000	5.000
0096	614.0951	Replacing Guardrail Rail and Hardware	LF	187.500	187.500
0098	618.0100	Maintenance And Repair of Haul Roads (project) 01. 5540-02-71	EACH	1.000	1.000

Estimate Of Quantities

5540-02-71

Line	Item	Item Description	Unit	Total	Qty
0100	619.1000	Mobilization	EACH	1.000	1.000
0102	624.0100	Water	MGAL	26.000	26.000
0104	625.0100	Topsoil	SY	25.000	25.000
0106	625.0500	Salvaged Topsoil	SY	424.000	424.000
0108	627.0200	Mulching	SY	425.000	425.000
0110	628.1504	Silt Fence	LF	2,550.000	2,550.000
0112	628.1520	Silt Fence Maintenance	LF	2,550.000	2,550.000
0114	628.1905	Mobilizations Erosion Control	EACH	8.000	8.000
0116	628.1910	Mobilizations Emergency Erosion Control	EACH	4.000	4.000
0118	628.2002	Erosion Mat Class I Type A	SY	24.000	24.000
0120	628.2027	Erosion Mat Class II Type C	SY	824.000	824.000
0122	628.7020	Inlet Protection Type D	EACH	8.000	8.000
0124	628.7555	Culvert Pipe Checks	EACH	110.000	110.000
0126	629.0210	Fertilizer Type B	CWT	2.000	2.000
0128	630.0140	Seeding Mixture No. 40	LB	9.000	9.000
0130	630.0200	Seeding Temporary	LB	13.000	13.000
0132	630.0500	Seed Water	MGAL	29.000	29.000
0134	633.5200	Markers Culvert End	EACH	27.000	27.000
0136	634.0616	Posts Wood 4x6-Inch X 16-FT	EACH	17.000	17.000
0138	634.0618	Posts Wood 4x6-Inch X 18-FT	EACH	3.000	3.000
0140	637.2230	Signs Type II Reflective F	SF	33.000	33.000
0142	638.2102	Moving Signs Type II	EACH	15.000	15.000
0144	638.3000	Removing Small Sign Supports	EACH	16.000	16.000
0146	642.5201	Field Office Type C	EACH	1.000	1.000
0148	643.0300	Traffic Control Drums	DAY	10,208.000	10,208.000
0150	643.0420	Traffic Control Barricades Type III	DAY	70.000	70.000
0152	643.0900	Traffic Control Signs	DAY	4,313.000	4,313.000
0154	643.1050	Traffic Control Signs PCMS	DAY	28.000	28.000
0156	643.3165	Temporary Marking Line Paint 6-Inch	LF	121,349.000	121,349.000
0158	643.3350	Temporary Marking Crosswalk Removable Tape 6-inch	LF	523.000	523.000
0160	643.5000	Traffic Control	EACH	1.000	1.000
0162	644.1430	Temporary Pedestrian Surface Plate	SF	143.000	143.000
0164	644.1440	Temporary Pedestrian Surface Matting	SF	260.000	260.000
0166	644.1601	Temporary Pedestrian Curb Ramp	DAY	126.000	126.000
0168	644.1605	Temporary Pedestrian Detectable Warning Field	SF	88.000	88.000
0170	644.1810	Temporary Pedestrian Barricade	LF	1,159.000	1,159.000
0172	645.0120	Geotextile Type HR	SY	73.000	73.000
0174	646.2020	Marking Line Epoxy 6-Inch	LF	81,570.000	81,570.000
0176	646.2040	Marking Line Grooved Wet Ref Epoxy 6-Inch	LF	31,936.000	31,936.000
0178	646.4020	Marking Line Epoxy 10-Inch	LF	201.000	201.000
0180	646.4720	Marking Line Same Day Epoxy 6-Inch	LF	47,131.000	47,131.000
0182	646.5020	Marking Arrow Epoxy	EACH	2.000	2.000
0184	646.6120	Marking Stop Line Epoxy 18-Inch	LF	69.000	69.000
0186	650.5500	Construction Staking Curb Gutter and Curb & Gutter	LF	1,323.000	1,323.000
0188	650.6000	Construction Staking Pipe Culverts	EACH	4.000	4.000
0190	650.8000	Construction Staking Resurfacing Reference	LF	44,402.000	44,402.000
0192	650.9000	Construction Staking Curb Ramps	EACH	17.000	17.000
0194	650.9500	Construction Staking Sidewalk (project) 01. 5540-02-71	EACH	1.000	1.000
0196	650.9911	Construction Staking Supplemental Control (project) 01. 5540-02-71	EACH	1.000	1.000

Estimate Of Quantities

5540-02-71

Line	Item	Item Description	Unit	Total	Qty
0198	690.0150	Sawing Asphalt	LF	1,989.000	1,989.000
0200	690.0250	Sawing Concrete	LF	224.000	224.000
0202	740.0440	Incentive IRI Ride	DOL	16,818.000	16,818.000
0204	ASP.1T0A	On-the-Job Training Apprentice at \$5.00/HR	HRS	700.000	700.000
0206	ASP.1T0G	On-the-Job Training Graduate at \$5.00/HR	HRS	475.000	475.000
0208	SPV.0060	Special 01. Reshape Embankment	EACH	34.000	34.000
0210	SPV.0060	Special 02. Adjusting Water Valve Box	EACH	20.000	20.000
0212	SPV.0090	Special 01. Regrade Ditch	LF	275.000	275.000
0214	SPV.0090	Special 02. Concrete Intersection Gutter Type D	LF	58.000	58.000
0216	SPV.0180	Special 01. Removing Distressed Pavement Milling	SY	5,063.000	5,063.000

CULVERT PIPE REMOVAL ITEMS

CAT	LOCATION	STA	203.0100	204.9060.S.01	REMARKS
			REMOVING SMALL PIPE CULVERTS	REMOVING APRON ENDWALL 24-INCH	
			EACH	EACH	
0010	STH 27 LT & RT	993+78	1	---	2 LF LT & 2 LF RT (24")
0010	STH 27 RT	1061+78	1	---	12 LF RT (30")
0010	STH 27 LT & RT	1100+22	---	2	CORRUGATED STEEL AEW
0010	STH 27 RT	1146+63	---	1	CORRUGATED STEEL AEW (RT)
0010	STH 27 RT	1157+38	---	1	CORRUGATED STEEL AEW (RT)
0010	STH 27 LT & RT	1243+81	1	---	2 LF LT & 4 LF RT (18")
0010	STH 27 LT & RT	1251+89	1	---	4 LF LT & 2 LF RT (18")
0010	STH 27 LT & RT	1276+17	1	---	2 LF LT & 4 LF RT (24")
0010	STH 27 LT & RT	1296+81	1	---	4 LF LT & 4 LF RT (18")
0010	STH 27 LT & RT	1326+78	1	---	2 LF LT & 4 LF RT (18")
0010	STH 27 LT & RT	1335+95	1	---	4 LF LT & 4 LF RT (18")
0010	STH 27 LT	1343+93	1	---	2 LF LT (24")
0010	STH 27 LT & RT	1352+78	1	---	4 LF LT & 2 LF RT (24")
0010	STH 27 LT & RT	1365+62	1	---	4 LF LT & 2 LF RT (24")
0010	STH 27 RT	1370+36	1	---	2 LF RT (24")
0010	STH 27 RT	1385+59	1	---	2 LF RT (24")
0010	STH 27 RT	1407+94	1	---	2 LF RT (24")
TOTAL			14	4	

ASPHALT & CONCRETE REMOVAL ITEMS

CAT	LOCATION	STA	TO	STA	204.0110	204.0115	204.0120	204.0140	204.0150	204.0155	REMARKS
					REMOVING ASPHALTIC SURFACE SY	REMOVING ASPHALTIC SURFACE BUTT JOINTS SY	REMOVING ASPHALTIC SURFACE MILLING SY	REMOVING GUTTER LF	REMOVING CURB & GUTTER LF	REMOVING CONCRETE SIDEWALK SY	
0010	STH 27	979+47.3	-	1021+50	35	144	16,069	---	---	---	SOUTH LIMITS TO CTH S
0010	STH 27	1021+50	-	1077+00	103	135	20,741	---	---	---	CTH S TO OLSEN FEED RD
0010	STH 27	1077+00	-	1164+00	228	70	30,439	---	---	---	OLSEN FEED RD TO ROUNDS RD
0010	STH 27	1164+00	-	1175+11.8	86	---	3,721	---	---	---	ROUNDS RD TO WEST ST
0010	STH 27	1175+11.8	-	1213+62.2	409	379	14,753	58	1,032	248	WEST ST TO HELGERSON ST
0010	STH 27	1213+62.2	-	1270+00	33	26	15,263	---	---	---	HELGERSON ST TO BROCKWAY LN
0010	STH 27	1270+00	-	1310+00	---	23	10,773	---	---	---	ROLLER COASTER RD TO HELLAND RD
0010	STH 27	1310+00	-	1335+00	144	23	7,247	---	---	---	HELLAND RD TO FRYSETH DR
0010	STH 27	1335+00	-	1387+00	14	25	13,958	---	---	---	FRYSETH DR TO BOB WHITE GLEN RD
0010	STH 27	1387+00	-	1423+49	133	85	10,433	---	---	---	BOB WHITE GLEN RD TO NORTH LIMITS
SUBTOTAL					1,185	910	143,397	58	1,032	248	
0020	STH 27	1175+11.8	-	1213+62.2	---	---	4,269	---	---	---	PARKING LANES
SUBTOTAL					0	0	4,269	0	0	0	
TOTAL					1,185	910	147,666	58	1,032	248	

AGGREGATE ITEMS

CAT	LOCATION	STA	TO	STA	305.0110	305.0120	624.0100	REMARKS
					BASE AGGREGATE DENSE 3/4-INCH	BASE AGGREGATE DENSE 1 1/4-INCH	WATER	
					TON	TON	MGAL	
0010	STH 27	979+47.3	-	1021+50	135	---	2	SOUTH LIMITS TO CTH S
0010	STH 27	1021+50	-	1077+00	193	---	3	CTH S TO OLSEN FEED RD
0010	STH 27	1077+00	-	1164+00	289	---	4	OLSEN FEED RD TO ROUNDS RD
0010	STH 27	1164+00	-	1175+11.8	40	---	1	ROUNDS RD TO WEST ST
0010	STH 27	1175+11.8	-	1213+62.2	15	513	8	WEST ST TO HELGERSON ST
0010	STH 27	1213+62.2	-	1270+00	144	---	2	HELGERSON ST TO BROCKWAY LN
0010	STH 27	1270+00	-	1310+00	103	---	2	ROLLER COASTER RD TO HELLAND RD
0010	STH 27	1310+00	-	1335+00	66	---	1	HELLAND RD TO FRYSETH DR
0010	STH 27	1335+00	-	1387+00	132	---	2	FRYSETH DR TO BOB WHITE GLEN RD
0010	STH 27	1387+00	-	1423+49	95	---	1	BOB WHITE GLEN RD TO NORTH LIMITS
TOTAL					1,212	513	26	

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ASPHALT ITEMS

CAT	LOCATION	STA	TO	STA	455.0605	460.5223	460.5224	465.0105	465.0120	465.0560	REMARKS
					TACK COAT	HMA PAVEMENT 3 LT 58-28 S	HMA PAVEMENT 4 LT 58-28 S	ASPHALTIC SURFACE	ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES	ASPHALTIC RUMBLE STRIPS, CENTERLINE	
					GAL	TON	TON	TON	TON	LF	
0010	STH 27	979+47.3	-	1021+50	1,935	2,023	1,589	---	6	2,150	SOUTH LIMITS TO CTH S
0010	STH 27	1021+50	-	1077+00	2,506	2,631	2,046	---	18	3,950	CTH S TO OLSEN FEED RD
0010	STH 27	1077+00	-	1164+00	3,662	3,845	2,990	---	39	7,788	OLSEN FEED RD TO ROUNDS RD
0010	STH 27	1164+00	-	1175+11.8	448	469	365	---	15	255	ROUNDS RD TO WEST ST
0010	STH 27	1175+11.8	-	1213+62.2	1,452	3,008	1,483	5	12	---	WEST ST TO HELGERSON ST
0010	STH 27	1213+62.2	-	1270+00	1,836	1,927	1,499	---	6	---	HELGERSON ST TO BROCKWAY LN
0010	STH 27	1270+00	-	1310+00	1,296	1,361	1,058	---	---	---	ROLLER COASTER RD TO HELLAND RD
0010	STH 27	1310+00	-	1335+00	873	916	713	---	25	---	HELLAND RD TO FRYSETH DR
0010	STH 27	1335+00	-	1387+00	1,679	1,762	1,371	---	3	---	FRYSETH DR TO BOB WHITE GLEN RD
0010	STH 27	1387+00	-	1423+49	1,263	1,326	1,031	---	23	---	BOB WHITE GLEN RD TO NORTH LIMITS
0010	STH 27	979+47.3	-	1175+11.8	159	---	---	167	---	---	DISTRESSED PAVEMENT MILLING
0010	STH 27	1213+62.2	-	1423+49	199	---	---	167	---	---	DISTRESSED PAVEMENT MILLING
SUBTOTAL					17,308	19,268	14,145	339	147	14,143	
0020	STH 27	1175+11.8	-	1213+62.2	214	---	419	---	---	---	PARKING LANES
SUBTOTAL					214	0	419	0	0	0	
TOTAL					17,522	19,268	14,564	339	147	14,143	

PWL TEST STRIPS

CAT	LOCATION	STA	TO	STA	460.0105.S	460.0110.S
					HMA PERCENT WITHIN LIMITS (PWL) TEST STRIP VOLUMETRICS	HMA PERCENT WITHIN LIMITS (PWL) TEST STRIP DENSITY
					EACH	EACH
0010	STH 27	979+47.3	-	1423+49	2	1
TOTAL					2	1

HMA QMP MIXTURE USE TABLE:

LOCATION	STATION	MIXTURE USE	UNDERLYING SURFACE	BID ITEM	TONS	THICKNESS	QUALITY MANAGEMENT PROGRAM TO BE USED:	
							MIXTURE ACCEPTANCE	DENSITY ACCEPTANCE
12 FT DRIVING LANES	STA 979+47.3 TO STA 1175+11.8	UPPER LAYER	HMA PAVEMENT 3 LT 58-28 S	HMA PAVEMENT 4 LT 58-28 S	5,114	1.75"	ITEM 460.2010 INCENTIVE AIR VOIDS HMA PAVEMENT	ITEM 460.2005 INCENTIVE DENSITY PWL HMA PAVEMENT
12 FT DRIVING LANES	STA 979+47.3 TO STA 1175+11.8	LOWER LAYER	MILLED SURFACE OR EXISTING BASE COURSE	HMA PAVEMENT 3 LT 58-28 S	6,575	2.25"	QMP AS PER STANDARD SPEC 465	ACCEPTANCE BY ORDINARY COMPACTION
12 FT DRIVING LANES	STA 1175+11.8 TO STA 1213+62.2	UPPER LAYER	HMA PAVEMENT 3 LT 58-28 S	HMA PAVEMENT 4 LT 58-28 S	1,022	1.75"	ITEM 460.2010 INCENTIVE AIR VOIDS HMA PAVEMENT	ITEM 460.2005 INCENTIVE DENSITY PWL HMA PAVEMENT
12 FT DRIVING LANES	STA 1175+11.8 TO STA 1213+62.2	LOWER LAYER	MILLED SURFACE OR EXISTING BASE COURSE	HMA PAVEMENT 3 LT 58-28 S	2,190	3.75"	QMP AS PER STANDARD SPEC 465	ACCEPTANCE BY ORDINARY COMPACTION
11 FT DRIVING LANES	STA 1213+62.2 TO STA 1423+49	UPPER LAYER	HMA PAVEMENT 3 LT 58-28 S	HMA PAVEMENT 4 LT 58-28 S	5,031	1.75"	ITEM 460.2010 INCENTIVE AIR VOIDS HMA PAVEMENT	ITEM 460.2005 INCENTIVE DENSITY PWL HMA PAVEMENT
11 FT DRIVING LANES	STA 1213+62.2 TO STA 1423+49	LOWER LAYER	MILLED SURFACE	HMA PAVEMENT 3 LT 58-28 S	6,469	2.25"	QMP AS PER STANDARD SPEC 465	ACCEPTANCE BY ORDINARY COMPACTION

HMA QMP MIXTURE USE TABLE:

LOCATION	STATION	MIXTURE USE	UNDERLYING SURFACE	BID ITEM	TONS	THICKNESS	QUALITY MANAGEMENT PROGRAM TO BE USED:	
							MIXTURE ACCEPTANCE	DENSITY ACCEPTANCE
8 FT SHOULDER	STA 979+47.3 RT TO STA 981+28.3 RT	UPPER LAYER	MILLED SURFACE	HMA PAVEMENT 4 LT 58-28 S	16	1.75"	ITEM 460.2010 INCENTIVE AIR VOIDS HMA PAVEMENT	ACCEPTANCE TESTING BY THE DEPARTMENT; NOT ELIGIBLE FOR INCENTIVE OR DISINCENTIVE
3 FT, 6 FT AND 8 FT SHOULDERS, TURN LANES AND INTERSECTIONS	STA 979+47.3 TO STA 1175+11.8	UPPER LAYER	HMA PAVEMENT 3 LT 58-28 S	HMA PAVEMENT 4 LT 58-28 S	1,860	1.75"	ITEM 460.2010 INCENTIVE AIR VOIDS HMA PAVEMENT	ACCEPTANCE TESTING BY THE DEPARTMENT; NOT ELIGIBLE FOR INCENTIVE OR DISINCENTIVE
3 FT, 6 FT AND 8 FT SHOULDERS, TURN LANES AND INTERSECTIONS	STA 979+47.3 TO STA 1175+11.8	LOWER LAYER	MILLED SURFACE	HMA PAVEMENT 3 LT 58-28 S	2,393	2.25"	QMP AS PER STANDARD SPEC 465	ACCEPTANCE BY ORDINARY COMPACTION
3 FT AND 10 FT SHOULDERS, AND INTERSECTIONS (WEST ST (SOUTH), STH 171 & 2ND ST)	STA 1175+11.8 TO STA 1213+62.2	UPPER LAYER	HMA PAVEMENT 3 LT 58-28 S	HMA PAVEMENT 4 LT 58-28 S	328	1.75"	ITEM 460.2010 INCENTIVE AIR VOIDS HMA PAVEMENT	ACCEPTANCE TESTING BY THE DEPARTMENT; NOT ELIGIBLE FOR INCENTIVE OR DISINCENTIVE
3 FT AND 10 FT SHOULDERS, AND INTERSECTIONS (WEST ST (SOUTH), STH 171 & 2ND ST)	STA 1175+11.8 TO STA 1213+62.2	LOWER LAYER	EXISTING BASE COURSE	HMA PAVEMENT 3 LT 58-28 S	818	3.75"	QMP AS PER STANDARD SPEC 465	ACCEPTANCE BY ORDINARY COMPACTION
INTERSECTIONS	STA 1175+11.8 TO STA 1213+62.2	UPPER LAYER	MILLED SURFACE	HMA PAVEMENT 4 LT 58-28 S	133	1.75"	ITEM 460.2010 INCENTIVE AIR VOIDS HMA PAVEMENT	ACCEPTANCE TESTING BY THE DEPARTMENT; NOT ELIGIBLE FOR INCENTIVE OR DISINCENTIVE
CURB & GUTTER AND GUTTER LOCATIONS	STA 1175+11.8 TO STA 1213+62.2	UPPER LAYER	BASE AGGREGATE DENSE 1 1/4-INCH	ASPHALTIC SURFACE	86	5.5"	QMP AS PER STANDARD SPEC 465	ACCEPTANCE BY ORDINARY COMPACTION
SIDEWALK TERRACE AND ENTRANCE/PARKING LOT LOCATIONS	STA 1175+11.8 TO STA 1213+62.2	UPPER LAYER	BASE AGGREGATE DENSE 1 1/4-INCH	ASPHALTIC SURFACE	5	3"	QMP AS PER STANDARD SPEC 465	ACCEPTANCE BY ORDINARY COMPACTION
1 FT, 4 FT, 7 FT AND 10 FT SHOULDERS AND INTERSECTIONS	STA 1213+62.2 TO STA 1423+49	UPPER LAYER	HMA PAVEMENT 3 LT 58-28 S	HMA PAVEMENT 4 LT 58-28 S	641	1.75"	ITEM 460.2010 INCENTIVE AIR VOIDS HMA PAVEMENT	ACCEPTANCE TESTING BY THE DEPARTMENT; NOT ELIGIBLE FOR INCENTIVE OR DISINCENTIVE
1 FT, 4 FT, 7 FT AND 10 FT SHOULDERS AND INTERSECTIONS	STA 1213+62.2 TO STA 1423+49	LOWER LAYER	MILLED SURFACE	HMA PAVEMENT 3 LT 58-28 S	823	2.25"	QMP AS PER STANDARD SPEC 465	ACCEPTANCE BY ORDINARY COMPACTION

HMA QMP MIXTURE USE TABLE:

LOCATION	STATION	MIXTURE USE	UNDERLYING SURFACE	BID ITEM	TONS	THICKNESS	QUALITY MANAGEMENT PROGRAM TO BE USED:	
							MIXTURE ACCEPTANCE	DENSITY ACCEPTANCE
8 FT PARKING LANES	STA 1175+11.8 TO STA 1213+62.2	UPPER LAYER	MILLED SURFACE	HMA PAVEMENT 4 LT 58-28 S	419	1.75"	ITEM 460.2010 INCENTIVE AIR VOIDS HMA PAVEMENT	ACCEPTANCE TESTING BY THE DEPARTMENT; NOT ELIGIBLE FOR INCENTIVE OR DISINCENTIVE

CULVERT PIPE ITEMS

CAT	LOCATION	STA	520.8700	520.9700.S.01 520.9700.S.02		520.9750.S	521.1018 521.1024 521.1030			521.3118 521.3124 521.3130			524.0624	633.5200	STEEL THICKNESS
			CLEANING CULVERT PIPES	CULVERT PIPE LINERS		CLEANING CULVERT PIPES FOR LINER VERIFICATION	APRON ENDWALLS FOR CULVERT PIPE STEEL			CULVERT PIPE CORRUGATED STEEL			APRON ENDWALLS FOR CULVERT PIPE SALVAGED 24-INCH	MARKERS CULVERT END	(INCHES)
			EACH	18-INCH LF	24-INCH LF	EACH	18-INCH EACH	24-INCH EACH	30-INCH EACH	18-INCH LF	24-INCH LF	30-INCH LF	EACH	EACH	
0010	STH 27 LT & RT	993+78	---	---	---	---	---	1	---	---	---	---	1	2	---
0010	STH 27 LT & RT	1007+36	---	---	200	1	---	---	---	---	---	---	1	1	---
0010	STH 27 LT	1024+32	---	---	---	---	---	---	---	---	---	---	1	1	---
0010	STH 27 LT & RT	1061+78	---	---	---	---	---	1	---	---	6	---	---	1	0.079
0010	STH 27 RT	1075+97	---	---	---	---	---	---	---	---	---	---	---	1	---
0010	STH 27 LT & RT	1100+22	---	---	200	1	---	2	---	---	---	---	---	2	---
0010	STH 27 LT & RT	1110+23	---	---	---	---	---	1	---	---	---	---	---	2	---
0010	STH 27 LT & RT	1116+62	---	---	---	---	---	---	---	---	---	---	2	---	---
0010	STH 27 RT	1129+08	---	---	---	---	---	---	---	---	---	---	1	---	---
0010	STH 27 RT	1135+33	---	---	---	---	---	---	---	---	---	---	---	1	---
0010	STH 27 RT	1146+63	---	---	---	---	---	1	---	---	---	---	---	---	---
0010	STH 27 RT	1157+38	---	---	---	---	---	1	---	---	---	---	---	---	---
0010	STH 27 LT	1197+15	1	---	---	---	---	---	---	---	---	---	---	1	---
0010	STH 27 LT & RT	1209+01	1	---	---	---	---	---	---	---	---	---	---	2	---
0010	STH 27 LT & RT	1243+81	---	---	---	---	2	---	---	---	---	---	---	2	---
0010	STH 27 LT & RT	1251+89	---	---	---	---	2	---	---	---	---	---	---	1	---
0010	STH 27 LT & RT	1276+17	---	---	---	---	---	2	---	---	---	---	---	2	---
0010	STH 27 LT & RT	1296+81	---	---	---	---	2	---	---	---	---	---	---	1	---
0010	STH 27 LT & RT	1326+78	---	---	---	---	2	---	---	---	---	---	---	---	---
0010	STH 27 LT & RT	1335+95	---	56	---	1	2	---	---	4	---	---	---	---	0.064
0010	STH 27 LT & RT	1343+93	---	---	48	1	---	2	---	---	---	---	---	---	---
0010	STH 27 LT & RT	1352+78	---	---	46	1	---	2	---	---	2	---	---	1	0.064
0010	STH 27 LT & RT	1365+62	---	---	40	1	---	2	---	---	---	---	---	2	---
0010	STH 27 RT	1370+36	---	---	---	---	---	2	---	---	---	---	---	---	---
0010	STH 27 LT & RT	1374+50	---	---	---	---	---	---	---	---	---	---	---	2	---
0010	STH 27 LT & RT	1385+59	---	---	---	---	---	2	---	---	---	---	---	2	---
0010	STH 27 RT	1407+94	---	---	---	---	---	2	---	---	2	---	---	---	0.064
TOTAL			2	56	534	6	10	20	1	4	4	6	6	27	

3

3

CONCRETE ITEMS

		416.0610		601.0411		601.0600		602.0405		602.0415		602.0505		602.0605		SPV.0090.02	
		DRILLED TIE BARS		CONCRETE CURB & GUTTER TYPE D 30-INCH		CONCRETE CURB PEDESTRIAN		CONCRETE SIDEWALK 4-INCH		CONCRETE SIDEWALK 6-INCH		CURB RAMP DETECTABLE WARNING FIELD YELLOW		CURB RAMP DETECTABLE WARNING FIELD RADIAL YELLOW		CONCRETE INTERSECTION GUTTER TYPE D	
CAT	LOCATION	STA	TO	STA	EACH	LF	LF	SF	SF	SF	SF	SF	SF	LF			
0010	STH 27	1175+11.8	-	1213+62.2	124	1,032	233	2,789	156	81	139	58					
TOTAL		124	1,032	233	2,789	156	81	139	58								

ADJUSTING ITEMS

		611.8110		SPV.0060.02		
		ADJUSTING MANHOLE COVERS		ADJUSTING WATER VALVE BOX		
CAT	LOCATION	STA	TO	STA	EACH	LF
0010	STH 27	1194+00	-	1213+62.2	3	---
SUBTOTAL					3	0
0030	STH 27	1175+11.8	-	1213+62.2	13	20
SUBTOTAL					13	20
TOTAL					16	20

GUARDRAIL ITEMS

		614.0400		614.0950		614.0951			
		ADJUSTING STEEL PLATE BEAM GUARD		REPLACING GUARDRAIL POSTS AND BLOCKS		REPLACING GUARDRAIL RAIL AND HARDWARE		REMARKS	
CAT	LOCATION	LT/RT	STA	TO	STA	LF	EACH	LF	
0010	STH 27	RT	992+49	-	996+98	---	1	37.5	1 DAMAGED BLOCK
0010	STH 27	RT	1003+00	-	1008+86	37.5	---	25	
0010	STH 27	LT	1021+14	-	1029+17	37.5	2	25	REATTACH END TERMINAL TO POST #1; INCIDENTAL TO REPLACING GUARDRAIL RAIL AND HARDWARE
0010	STH 27	RT	1072+66	-	1078+02	---	1	37.5	1 DETACHED BLOCK
0010	STH 27	LT	1097+19	-	1101+50	25	1	25	
0010	STH 27	RT	1097+18	-	1102+63	---	---	12.5	
0010	STH 27	RT	1108+71	-	1110+42	---	---	12.5	
0010	STH 27	LT	1108+98	-	1112+42	25	---	---	
0010	STH 27	LT	1121+45	-	1123+15	25	---	12.5	
TOTAL		150	5	187.5					

EROSION CONTROL AND LANDSCAPING ITEMS

CAT	LOCATION	LT/RT	STA	TO STA	201.0105	201.0205	606.0200		625.0500	625.0100	627.0200	628.1504	628.1520	628.1905	628.1910	628.2002	628.2027	628.7020	628.7555	629.0210	630.0140	630.0200	630.0500	645.0120	SPV.0060.01	SPV.0090.01
					CLEARING STA	GRUBBING STA	CLEARING AND GRUBBING LT/RT	RIP RAP CY	SALVAGED TOPSOIL SY	TOPSOIL SY	MULCHING SY	SILT FENCE LF	SILT FENCE MAINTENANCE LF	MOBILIZATIONS EROSION CONTROL EACH	MOBILIZATIONS EROSION CONTROL EMERGENCY EACH	EROSION MAT CLASS I TYPE A SY	EROSION MAT URBAN CLASS II TYPE C SY	INLET PROTECTION TYPE D EACH	CULVERT PIPE CHECKS EACH	FERTILIZER TYPE B CWT	SEEDING MIXTURE NO. 40 LB	SEEDING TEMPORARY LB	SEED WATER MGAL	GEOTEXTILE TYPE HR SY	RESHAPE EMBANKMENT EACH	REGRADE DITCH LF
0010	STH 27	LT & RT	993+78		1	1	RT	6.5	---	---	---	120	120	---	---	---	43	---	4	---	---	---	1.0	13	2	25
0010	STH 27	LT & RT	1007+36		1	1	RT	6.5	---	---	---	50	50	---	---	---	27	---	4	---	---	---	0.6	13	1	25
0010	STH 27	LT	1024+32		1	1	LT	6.5	---	---	---	50	50	---	---	---	27	---	4	---	---	---	0.6	13	1	25
0010	STH 27	LT & RT	1061+78		1	1	RT	9.0	---	---	---	80	80	---	---	---	66	---	4	---	---	---	1.5	18	1	50
0010	STH 27	LT & RT	1100+22		---	---	---	---	---	---	---	100	100	---	---	---	20	---	4	---	---	---	0.5	---	2	---
0010	STH 27	RT	1110+23		---	---	---	---	---	---	---	50	50	---	---	---	10	---	4	---	---	---	0.2	---	1	---
0010	STH 27	LT & RT	1116+62		---	---	---	---	---	---	---	100	100	---	---	---	20	---	4	---	---	---	0.5	---	2	---
0010	STH 27	RT	1129+08		---	---	---	---	---	---	---	50	50	---	---	---	10	---	4	---	---	---	0.2	---	1	---
0010	STH 27	RT	1146+63		---	---	---	---	---	---	---	50	50	---	---	---	10	---	4	---	---	---	0.2	---	1	---
0010	STH 27	RT	1157+38		---	---	---	---	---	---	---	50	50	---	---	---	10	---	4	---	---	---	0.2	---	1	---
0010	STH 27	LT & RT	1175+11.8 - 1213+62.2		---	---	---	---	339	20	340	---	---	---	19	---	8	---	1	7	10	8.1	---	---	---	---
0010	STH 27	LT & RT	1243+81		---	---	---	---	---	---	---	130	130	---	---	---	42	---	4	---	---	---	1.0	---	2	25
0010	STH 27	LT & RT	1251+89		---	---	---	---	---	---	---	130	130	---	---	---	25	---	4	---	---	---	0.6	---	2	---
0010	STH 27	LT & RT	1276+17		---	---	---	2.5	---	---	---	130	130	---	---	---	34	---	4	---	---	---	0.8	5	2	---
0010	STH 27	LT & RT	1296+81		---	---	---	3.3	---	---	---	140	140	---	---	---	38	---	4	---	---	---	0.9	7	2	---
0010	STH 27	LT & RT	1326+78		---	---	---	---	---	---	---	120	120	---	---	---	30	---	4	---	---	---	0.7	---	2	---
0010	STH 27	LT & RT	1335+95		---	---	---	---	---	---	---	140	140	---	---	---	49	---	4	---	---	---	1.1	---	2	25
0010	STH 27	LT & RT	1343+93		---	---	---	---	---	---	---	110	110	---	---	---	40	---	4	---	---	---	0.9	---	2	25
0010	STH 27	LT & RT	1352+78		1	1	LT	1.7	---	---	---	130	130	---	---	---	51	---	4	---	---	---	1.2	4	2	25
0010	STH 27	LT & RT	1365+62		---	---	---	---	---	---	---	130	130	---	---	---	51	---	4	---	---	---	1.2	---	2	25
0010	STH 27	RT	1370+36		---	---	---	---	---	---	---	60	60	---	---	---	13	---	4	---	---	---	0.3	---	1	---
0010	STH 27	RT	1385+59		---	---	---	---	---	---	---	60	60	---	---	---	30	---	4	---	---	---	0.7	---	1	25
0010	STH 27	RT	1407+94		---	---	---	---	---	---	---	60	60	---	---	---	13	---	4	---	---	---	0.3	---	1	---
SUBTOTAL					5	5	---	36	339	20	340	2,040	2,040	0	0	19	659	8	88	1	7	10	23	73	34	275
0010	STH 27	LT/RT	UNDISTRIBUTED		---	---	---	---	85	5	85	510	510	8	4	5	165	---	22	1	2	3	6	---	---	---
SUBTOTAL					0	0	---	0	85	5	85	510	510	8	4	5	165	0	22	1	2	3	6	0	0	0
TOTAL					5	5	---	36	424	25	425	2,550	2,550	8	4	24	824	8	110	2	9	13	29	73	34	275

TRAFFIC CONTROL ITEMS

CAT	LOCATION	SEQUENCE	DURATION (DAYS)	643.0300		643.0420		643.0900		643.1050		644.1430	644.1440	644.1601		644.1605	644.1810	
				TRAFFIC CONTROL DRUMS	TRAFFIC CONTROL BARRICADES TYPE III	TRAFFIC CONTROL SIGNS	TRAFFIC CONTROL SIGNS PCMS	TEMPORARY PEDESTRIAN SURFACE PLATE	TEMPORARY PEDESTRIAN SURFACE MATTING	TEMPORARY PEDESTRIAN CURB RAMP	TEMPORARY PEDESTRIAN DETECTABLE WARNING FIELD	TEMPORARY PEDESTRIAN BARRICADE						
				EACH	DAY	EACH	DAY	EACH	DAY	UNITS	DAY	SF	SF	EACH	DAY	SF	LF	
STAGE 1 - VILLAGE OF MT STERLING																		
0010	STH 27 & STH 171	PROJECT ADVANCED WARNING	7	20	140	---	---	---	---	4	28	---	---	---	---	---	---	
0010	STA 1175+11.8 TO STA 1213+62.8	SIDEROAD ADVANCED WARNING	32	---	---	---	---	11	352	---	---	---	---	---	---	---	---	
0010	STAGE 1 PEDESTRIAN ACCOMMODATIONS	STH 27 SB PARKING LANE CLOSURE	14	249	3,486	5	70	31	434	---	---	113	182	7	98	56	811	
0010	STAGE 2 PEDESTRIAN ACCOMMODATIONS	STH 27 NB PARKING LANE CLOSURE	10	207	2,070	---	---	8	80	---	---	15	78	2	20	24	234	
0010	STAGE 3 PEDESTRIAN ACCOMMODATIONS	2ND STREET/STH 171 SHOULDER CLOSURE	8	34	272	---	---	18	144	---	---	15	---	1	8	8	114	
SUBTOTAL					5,968		70		1,010		28		143		126		88	1,159
STAGE 2 - SOUTH LIMITS TO NORTH LIMITS																		
0010	STH 27	SIDEROAD ADVANCED WARNING	61	---	---	---	---	31	1891	---	---	---	---	---	---	---	---	
0010	STH 27	MOVING OPERATIONS ADVANCED WARNING	61	---	---	---	---	10	610	---	---	---	---	---	---	---	---	
0010	STH 27	MILLED SURFACE SIGNING ON STH 27	15	---	---	---	---	39	585	---	---	---	---	---	---	---	---	
0010	STH 27	DROP OFF SIGNING ON STH 27	20	212	4,240	---	---	8	160	---	---	---	---	---	---	---	---	
0010	STA 1175+11.8 TO STA 1213+62.8	LOOSE GRAVEL SIGNING ON STH 27	3	---	---	---	---	19	57	---	---	---	---	---	---	---	---	
SUBTOTAL					4,240		0		3,303		0		0		0		0	0
TOTAL					10,208		70		4,313		28		143		126		88	1,159

PAVEMENT MARKING ITEMS

CAT	LOCATION	STA	TO	STA	643.3165	643.3350	646.2020		646.2040		646.4020	646.4720	646.5020	646.6120	REMARKS
					TEMPORARY MARKING LINE PAINT 6-INCH (YELLOW)	TEMPORARY MARKING LINE CROSSWALK TAPE 6-INCH (WHITE)	MARKING LINE EPOXY 6-INCH (WHITE)	MARKING LINE EPOXY 6-INCH (YELLOW)	MARKING LINE GROOVED WET REF EPOXY 6-INCH (YELLOW)	MARKING LINE EPOXY 10-INCH (WHITE)	MARKING LINE SAME DAY EPOXY 6-INCH (YELLOW)	MARKING ARROW EPOXY (WHITE)	MARKING LINE STOP LINE EPOXY 18-INCH (WHITE)		
					LF	LF	LF	LF	LF	LF	LF	LF	EACH	LF	
0010	STH 27	979+47.3	-	1021+50	15,334	---	7,995	---	6,928	115	313	2	---	---	SOUTH LIMITS TO CTH S
0010	STH 27	1021+50	-	1077+00	19,876	---	10,579	---	8,776	---	---	---	---	---	CTH S TO OLSEN FEED RD
0010	STH 27	1077+00	-	1164+00	33,000	---	17,142	---	15,600	---	---	---	---	---	OLSEN FEED RD TO ROUNDS RD
0010	STH 27	1164+00	-	1175+11.8	2,856	---	2,224	---	632	---	247	---	---	---	ROUNDS RD TO WEST ST
0010	STH 27	1175+11.8	-	1213+62.2	7,701	523	1,769	---	---	---	6,322	---	---	---	WEST ST TO HELGERSON ST
0010	STH 27	1213+62.2	-	1270+00	11,276	---	11,209	---	---	---	9,551	---	---	---	HELGERSON ST TO BROCKWAY LN
0010	STH 27	1270+00	-	1310+00	8,000	---	7,931	---	---	---	8,000	---	---	---	ROLLER COASTER RD TO HELLAND RD
0010	STH 27	1310+00	-	1335+00	5,000	---	4,941	---	---	---	5,000	---	---	---	HELLAND RD TO FRYSETH DR
0010	STH 27	1335+00	-	1387+00	10,400	---	10,309	---	---	---	10,400	---	---	---	FRYSETH DR TO BOB WHITE GLEN RD
0010	STH 27	1387+00	-	1423+49	7,298	---	7,145	---	---	---	7,298	---	---	---	BOB WHITE GLEN RD TO NORTH LIMITS
0010	STH 171	98+51.5	-	100+66	608	---	22	304	---	86	---	---	---	69	
SUBTOTAL					121,349	523	81,570	31,936	201	47,131	2	69			
0010	STH 27	PROJECT			---	---	---	---	---	---	---	---	---	---	
SUBTOTAL					0	0	0	0	0	0	0	0	0	0	
TOTAL					121,349	523	81,570	31,936	201	47,131	2	69			

**NO TEMPORARY MARKINGS ON MILLED SURFACES UNLESS DIRECTED BY ENGINEER IN THE FIELD
DOUBLE YELLOW CENTERLINE TEMPORARY MARKINGS ON LOWER HMA LAYERS**

CONSTRUCTION STAKING ITEMS

CAT	LOCATION	STA	TO STA	650.5500	650.6000	650.8000	650.9000	650.9500	650.9911
				CONSTRUCTION STAKING CURB GUTTER AND CURB & GUTTER LF	CONSTRUCTION STAKING PIPE CULVERTS EACH	CONSTRUCTION STAKING RESURFACING REFERENCE LF	CONSTRUCTION STAKING CURB RAMPS EACH	CONSTRUCTION STAKING SIDEWALK (5540-02-71) EACH	CONSTRUCTION STAKING SUPPLEMENTAL CONTROL (5540-02-71) EACH
0010	STH 27	1061+78		---	1	---	---	---	---
0010	STH 27	1175+11.8 - 1213+62.2		1,323	---	---	17	---	---
0010	STH 27	1335+95		---	1	---	---	---	---
0010	STH 27	1352+78		---	1	---	---	---	---
0010	STH 27	1407+94		---	1	---	---	---	---
SUBTOTAL				1,323	4	0	17	0	0
0010	STH 27	PROJECT		---	---	44,402	---	1	1
SUBTOTAL				0	0	44,402	0	1	1
TOTAL				1,323	4	44,402	17	1	1

SIGNING ITEMS

CAT	LOCATION	STA	OFFSET	SIGN CODE	W x H	634.0616		634.0618		637.2230		638.2102		638.3000		REMARKS
						POSTS WOOD 4X6-INCH X 16-FT	EACH	POSTS WOOD 4X6-INCH X 18-FT	EACH	SIGNS TYPE II REFLECTIVE F	SF	MOVING SIGNS TYPE II	EACH	REMOVING SMALL SIGN SUPPORTS	EACH	
0010	STH 27	997+00	25' LT	W14-3	36" X 48"	1	---	---	---	---	1	1	1	1	VERIFY EXISTING LOCATION	
0010	STH 27	1025+13	25' LT	W14-3	36" X 48"	1	---	---	---	---	1	1	1	1	VERIFY EXISTING LOCATION	
0010	STH 27	1027+65	25' RT	W14-3	36" X 48"	1	---	---	---	---	1	1	1	1	VERIFY EXISTING LOCATION	
0010	STH 27	1073+00	25' RT	W14-3	36" X 48"	1	---	---	---	---	1	1	1	1	VERIFY EXISTING LOCATION	
0010	STH 27	1082+72	25' LT	W14-3	36" X 48"	1	---	---	---	---	1	1	1	1	VERIFY EXISTING LOCATION	
0010	STH 27	1165+11	25' RT	W14-3	36" X 48"	1	---	---	---	---	1	1	1	1	VERIFY EXISTING LOCATION	
0010	STH 27	1182+80	25' LT	W14-3	36" X 48"	1	---	---	---	---	1	1	1	1	VERIFY EXISTING LOCATION	
0010	STH 27	1187+91	29.5' RT	W11-2	30" X "30"	1	---	---	6.25	---	---	---	---	---		
0010	STH 27	1187+91	29.5' RT	W16-7L	24" X 12"	---	---	---	2	---	---	---	---	---		
0010	STH 27	1188+03	26.3' RT	J2-3	72" X 57"	---	---	2	---	---	1	2	2	2		
0010	STH 27	1189+28	30' LT	J2-3	72" X 57"	---	---	1	---	---	1	1	1	1		
0010	STH 27	1189+36	38.1' RT	R1-1	30" X 30"	1	---	---	---	---	1	1	1	1		
0010	STH 27	1189+44	29.8' RT	R5-1	30" X 30"	1	---	---	---	---	1	1	1	1		
0010	STH 27	1189+51	31.3' LT	W11-2	30" X "30"	1	---	---	6.25	---	---	---	---	---		
0010	STH 27	1189+51	31.3' LT	W16-7L	24" X 12"	---	---	---	2	---	---	---	---	---		
0010	STH 27	1192+30	31.3' LT	J2-2	48" X 57"	1	---	---	---	---	1	1	1	1		
0010	STH 27	1232+19	20' RT	W14-3	36" X 48"	1	---	---	---	---	1	1	1	1	VERIFY EXISTING LOCATION	
0010	STH 27	1235+69	20' LT	W14-3	36" X 48"	1	---	---	---	---	1	1	1	1	VERIFY EXISTING LOCATION	
0010	STH 171	98+25	26' RT	W11-2	30" X "30"	1	---	---	6.25	---	---	---	---	---		
0010	STH 171	98+25	26' RT	W16-7L	24" X 12"	---	---	---	2	---	---	---	---	---		
0010	STH 171	98+43	28.7' LT	W11-2	30" X "30"	1	---	---	6.25	---	---	---	---	---		
0010	STH 171	98+43	28.7' LT	W16-7L	24" X 12"	---	---	---	2	---	---	---	---	---		
0010	STH 171	98+76	19.6' LT	J4-1	24" X 36"	1	---	---	---	---	1	1	1	1	VERIFY EXISTING LOCATION	
TOTAL						17	3	33	15	16						

SAWING ITEMS

690.0150 690.0250
 SAWING SAWING
 ASPHALT CONCRETE

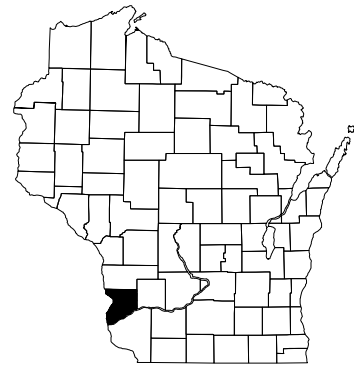
CAT	LOCATION	STA	TO	STA	LF	LF	REMARKS
0010	STH 27	979+47.3	-	1021+50	26	---	SOUTH LIMITS TO CTH S
0010	STH 27	1021+50	-	1077+00	35	---	CTH S TO OLSEN FEED RD
0010	STH 27	1077+00	-	1164+00	47	---	OLSEN FEED RD TO ROUNDS RD
0010	STH 27	1164+00	-	1175+11.8	133	---	ROUNDS RD TO WEST ST
0010	STH 27	1175+11.8	-	1213+62.2	1,610	224	WEST ST TO HELGERSON ST
0010	STH 27	1213+62.2	-	1270+00	48	---	HELGERSON ST TO BROCKWAY LN
0010	STH 27	1270+00	-	1310+00	---	---	ROLLER COASTER RD TO HELLAND RD
0010	STH 27	1310+00	-	1335+00	47	---	HELLAND RD TO FRYSETH DR
0010	STH 27	1335+00	-	1387+00	18	---	FRYSETH DR TO BOB WHITE GLEN RD
0010	STH 27	1387+00	-	1423+49	25	---	BOB WHITE GLEN RD TO NORTH LIMITS
TOTAL					1,989	224	

DISTRESSED PAVEMENT MILLING

SPV. 0180.01
 REMOVING
 DISTRESSED
 PAVEMENT
 MILLING

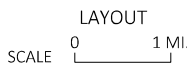
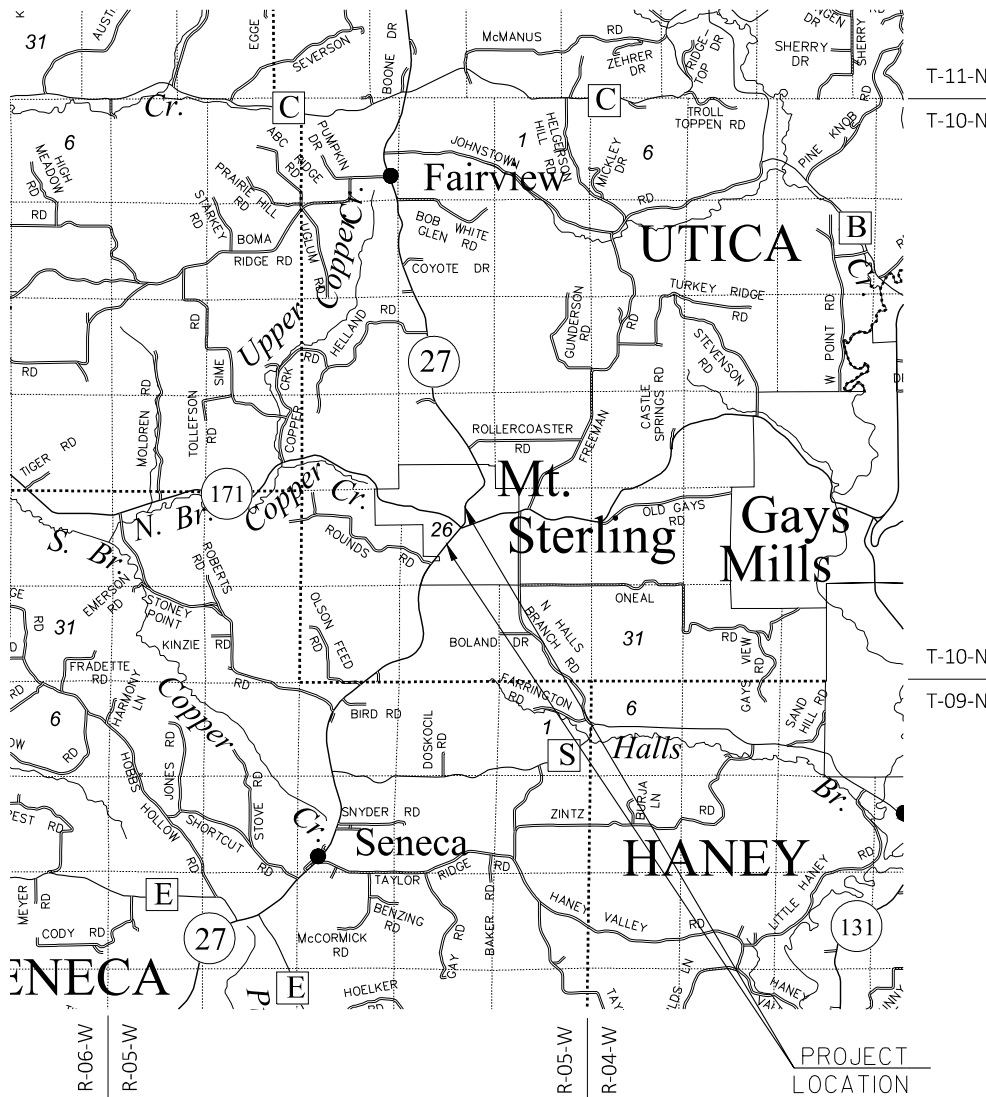
CAT	LOCATION	STA	TO	STA	SY	REMARKS
0010	STH 27	979+47.3	-	1054+00	1,373	
0010	STH 27	1071+00	-	1088+00	337	
0010	STH 27	1107+00	-	1125+00	318	
0010	STH 27	1145+00	-	1159+00	234	
0010	STH 27	1213+62	-	1416+62	2,801	
TOTAL					5,063	

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION TRANSPORTATION PROJECT PLAT TITLE SHEET 5540-02-21 PRAIRIE DU CHIEN - VIROQUA



0.1 MI N NORTH AVE TO CTH C

STH 27 CRAWFORD COUNTY



CONVENTIONAL SYMBOLS	
SECTION LINE	--- ---
QUARTER LINE	--- ---
SIXTEENTH LINE	--- ---
NEW REFERENCE LINE	--- ---
NEW R/W LINE	--- ---
EXISTING R/W OR HE LINE	--- ---
PROPERTY LINE	--- ---
LOT, TIE & OTHER MINOR LINES	--- ---
SLOPE INTERCEPT	--- ---
CORPORATE LIMITS	--- ---
UNDERGROUND FACILITY (COMMUNICATIONS, ELECTRIC, ETC)	--- ---
NEW R/W (FEE OR HE) (HATCHING VARIES BY OWNER)	--- ---
TEMPORARY LIMITED EASEMENT AREA	--- ---
EASEMENT AREA (PERMANENT LIMITED OR RESTRICTED DEVELOPMENT)	--- ---
TRANSMISSION STRUCTURES	--- ---
BUILDING TO BE REMOVED	--- ---
BRIDGE	--- ---
CULVERT	--- ---

CONVENTIONAL ABBREVIATIONS	
ACCESS RIGHTS	AR
ACRES	AC
AHEAD	AH
ALUMINUM	ALUM
AND OTHERS	ET AL
BACK	BK
BLOCK	BLK
CENTERLINE	C/L
CERTIFIED SURVEY MAP	CSM
CONCRETE	CONC
COUNTY	CO
COUNTY TRUNK HIGHWAY	CTH
DISTANCE	DIST
CORNER	COR
DOCUMENT NUMBER	DOC
EASEMENT	EASE
EXISTING	EX
GAS VALVE	GV
GRID NORTH	GN
HIGHWAY EASEMENT	HE
IDENTIFICATION	ID
LAND CONTRACT	LC
LEFT	LT
MONUMENT	MON
NATIONAL GEODETIC SURVEY	NGS
NUMBER	NO
OUTLOT	OL
PAGE	P
POINT OF TANGENCY	PT
PERMANENT LIMITED EASEMENT	PLE
POINT OF BEGINNING	POB
POINT OF CURVATURE	PC
POINT OF COMPOUND CURVE	PCC

CONVENTIONAL ABBREVIATIONS	
ACCESS RESTRICTED BY ACQUISITION	--- ---
NO ACCESS (BY STATUTORY AUTHORITY)	--- ---
ACCESS RESTRICTED (BY PREVIOUS PROJECT OR CONTROL)	--- ---
NO ACCESS (NEW HIGHWAY)	--- ---
PARCEL NUMBER	(25)
UTILITY NUMBER	(40)
PARALLEL OFFSETS	--- ---

CONVENTIONAL ABBREVIATIONS	
ACCESS RIGHTS	AR
ACRES	AC
AHEAD	AH
ALUMINUM	ALUM
AND OTHERS	ET AL
BACK	BK
BLOCK	BLK
CENTERLINE	C/L
CERTIFIED SURVEY MAP	CSM
CONCRETE	CONC
COUNTY	CO
COUNTY TRUNK HIGHWAY	CTH
DISTANCE	DIST
CORNER	COR
DOCUMENT NUMBER	DOC
EASEMENT	EASE
EXISTING	EX
GAS VALVE	GV
GRID NORTH	GN
HIGHWAY EASEMENT	HE
IDENTIFICATION	ID
LAND CONTRACT	LC
LEFT	LT
MONUMENT	MON
NATIONAL GEODETIC SURVEY	NGS
NUMBER	NO
OUTLOT	OL
PAGE	P
POINT OF TANGENCY	PT
PERMANENT LIMITED EASEMENT	PLE
POINT OF BEGINNING	POB
POINT OF CURVATURE	PC
POINT OF COMPOUND CURVE	PCC

CURVE DATA	
LONG CHORD	LCH
LONG CHORD BEARING	LCB
RADIUS	R
DEGREE OF CURVE	D
CENTRAL ANGLE	Δ/DELTA
LENGTH OF CURVE	L
TANGENT	T
DIRECTION AHEAD	DA
DIRECTION BACK	DB

CONVENTIONAL UTILITY SYMBOLS	
WATER	--- ---
GAS	--- ---
TELEPHONE OVERHEAD	--- ---
TRANSMISSION LINES	--- ---
ELECTRIC	--- ---
CABLE TELEVISION	--- ---
FIBER OPTIC	--- ---
SANITARY SEWER	--- ---
STORM SEWER	--- ---

THE NOTES, CONVENTIONAL SIGNS, AND ABBREVIATIONS ARE ASSOCIATED WITH EACH TRANSPORTATION PROJECT PLAT FOR PROJECT 5540-02-21

- NOTES:**
- POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), CRAWFORD COUNTY, NAD83 (2011) IN US SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.
 - ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 3/4" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.
 - ALL RIGHT-OF-WAY LINES DEPICTED IN THE NON-ACQUISITION AREAS ARE INTENDED TO RE-ESTABLISH EXISTING RIGHT-OF-WAY LINES AS DETERMINED FROM PREVIOUS PROJECTS, OTHER RECORDED DOCUMENTS, OR FROM CENTERLINE OF EXISTING PAVEMENTS.
 - RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS" OF PUBLIC RECORD.
 - DIMENSIONING FOR THE NEW RIGHT-OF-WAY IS MEASURED ALONG AND PERPENDICULAR TO THE NEW REFERENCE LINES.
 - A TEMPORARY LIMITED EASEMENT (TLE) IS A RIGHT FOR CONSTRUCTION PURPOSES, AS DEFINED HEREIN, INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON, THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM DESIRABLE. ALL (TLEs) ON THIS PLAT EXPIRE AT THE COMPLETION OF THE CONSTRUCTION PROJECT FOR WHICH THIS INSTRUMENT IS GIVEN.
 - A PERMANENT LIMITED EASEMENT (PLE) IS A RIGHT FOR CONSTRUCTION AND MAINTENANCE PURPOSES, AS DEFINED HEREIN, INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON AND THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM DESIRABLE, BUT WITHOUT PREJUDICE TO THE OWNER'S RIGHTS TO MAKE OR CONSTRUCT IMPROVEMENTS ON SAID LANDS OR TO FLATTEN THE SLOPES, PROVIDING SAID ACTIVITIES WILL NOT IMPAIR OR OTHERWISE ADVERSELY AFFECT THE HIGHWAY FACILITIES.
 - PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM DATA DERIVED FROM MAPS AND DOCUMENTS OF PUBLIC RECORD AND/OR EXISTING OCCUPATIONAL LINES. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.
 - FOR THE LATEST ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN LACROSSE.
 - PARCEL IDENTIFICATION NUMBERS MAY NOT POINT TO ALL AREAS OF ACQUISITION, AS NOTED ON THE SCHEDULE OF LANDS & INTERESTS REQUIRED.
 - INFORMATION FOR THE BASIS OF EXISTING HIGHWAY RIGHT-OF-WAY POINTS OF REFERENCE AND ACCESS CONTROL ARE LISTED ON THE TPP DETAIL PAGES.

PROJECT NUMBER 5540-02-21 - 4.01
SHEET 2 OF 2

RECORDED 11/30/2021 09:52 AM
MELISSA C NAGEL
REGISTER OF DEEDS OFFICE
CRAWFORD COUNTY, WI
RECORDING FEE 25.00
PAGES: 2
VOLUME: 3PL PAGE: 126
**The above recording information
verifies that this document has
been electronically recorded and
returned to the submitter.**

RESERVED FOR REGISTER OF DEEDS
PROJECT NUMBER 5540-02-21-4.01
SHEET 1 OF 2

TRANSPORTATION PROJECT PLAT NO: 5540-02-21 - 4.01

THAT PART OF LOT 1 IN BLOCK 1, AND PART OF LOT 1 AND LOT 2 IN BLOCK 6, AND PART OF LOT 1 IN BLOCK 7, AND THAT PART OF VACATED SECOND STREET, ALL IN THE ORIGINAL PLAT OF MOUNT STERLING, BEING IN AND INCLUDING PART OF THE SOUTHWEST 1/4 OF THE NORTHEAST 1/4, ALL IN SECTION 26, TOWN 10 NORTH, RANGE 5 WEST, VILLAGE OF MOUNT STERLING, CRAWFORD COUNTY, WISCONSIN.

RELOCATION ORDER STH 27 PRAIRIE DU CHIEN - VIROQUA (0.1 MI N NORTH AVE TO CTH C) CRAWFORD COUNTY

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE PROJECT.

TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SUBSECTIONS 84.02 (3), 84.09, AND 84.30, WISCONSIN STATUTES, THE DEPARTMENT OF TRANSPORTATION HEREBY ORDERS THAT:

1. THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS LAID OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE PROJECT.
2. THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE DEPARTMENT FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE STATE OF WISCONSIN, PURSUANT TO THE PROVISIONS OF SUBSECTION 84.09 (1) OR (2), WISCONSIN STATUTES.

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), CRAWFORD COUNTY, NAD83 (2011) IN US SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE:

EXISTING HIGHWAY RIGHT-OF-WAY FOR STH 27/MAIN ST ESTABLISHED FROM PREVIOUS PROJECT 5543-01-23, 5545-02-21, TO380(6)

EXISTING HIGHWAY RIGHT-OF-WAY FOR FIRST ST ESTABLISHED FROM CSM 100(NOT SHOWN IN DETAIL), CSM 775(NOT SHOWN IN DETAIL), AND PLAT OF SURVEY 1971 (RAHA)(NOT SHOWN IN DETAIL), ORIGINAL PLAT OF MOUNT STERLING.

EXISTING HIGHWAY RIGHT-OF-WAY FOR STH 171/SECOND ST ESTABLISHED FROM PREVIOUS PROJECT 5543-01-23, 5545-02-21, TO380(6), DJ-9279A, CSM 35 DOC 165283 (NOT SHOWN IN DETAIL), ORIGINAL PLAT OF MOUNT STERLING.

EXISTING HIGHWAY RIGHT-OF-WAY FOR SECOND ST ESTABLISHED FROM EXISTING CENTERLINE WITH A WIDTH OF 60' PER ORIGINAL PLAT OF MOUNT STERLING.

EXISTING HIGHWAY RIGHT-OF-WAY FOR STH 171/DIAGONAL ST ESTABLISHED FROM PREVIOUS PROJECT 5543-01-23, PLAT OF SURVEY 2013 (WARNER)(NOT SHOWN IN DETAIL), ORIGINAL PLAT OF MOUNT STERLING.

FOR THE LATEST ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN LA CROSSE.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 3/4" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

NGS CONTROL POINT UTILIZED: PID DHS354, DESIGNATION: MOUNT STERLING GPS, CRAWFORD COUNTY, WI.

REFER TO TITLE SHEET, RECORDED AS SHEET 2 OF 2 OF THIS DOCUMENT FOR ADDITIONAL INFORMATION.

RIGHT OF WAY POINT TABLE				
POINT	STATION	OFFSET	Y COORDS	X COORDS
100	1185+19.74	0.00'	218003.487	375540.936
101	1185+19.60	33.18' LT	218025.127	375515.780
102	1185+79.60	33.19' LT	218070.459	375555.088
103	1187+74.72	33.23' LT	218217.877	375682.914
104	1187+75.42	58.42' LT	218234.907	375664.347
105	1188+79.38	55.53' LT	218311.549	375734.647
106	1188+91.27	33.26' LT	218305.930	375759.264
107	1190+13.88	33.28' LT	218398.569	375839.591
108	1190+13.89	0.00'	218376.766	375864.735
109	1190+13.90	32.72' RT	218355.331	375889.456
110	1189+52.13	32.73' RT	218308.664	375848.991
IP111	1189+52.10	66.28' RT	218286.664	375874.313
112	1189+21.14	124.56' RT	218225.081	375898.053
113	1188+56.54	90.25' RT	218198.772	375829.806
114	1188+03.52	62.08' RT	218177.174	375773.779
115	1187+48.35	32.77' RT	218154.704	375715.493
116	1185+79.88	32.81' RT	218027.417	375605.122
117	1185+19.88	32.82' RT	217982.085	375565.815

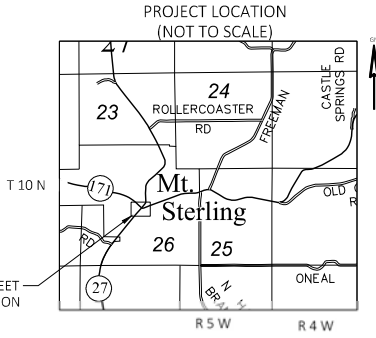
COURSE TABLE		
COURSE	BEARING	DISTANCE
100 - 101	N49° 17' 48"W	33.18'
101 - 102	N40° 55' 43"E	60.00'
102 - 103	N40° 55' 43"E	195.12'
103 - 104	N47° 28' 17"W	25.19'
104 - 105	N42° 31' 43"E	104.00'
105 - 106	S77° 08' 37"E	25.25'
106 - 107	N40° 55' 43"E	122.61'
107 - 108	S49° 04' 17"E	33.28'
108 - 109	S49° 04' 17"E	32.72'
109 - 110	S40° 55' 43"W	61.77'
110 - IP111	S49° 00' 59"E	33.54'
IP111 - 112	S21° 04' 54"E	66.00'
112 - 113	S68° 55' 06"W	73.14'
113 - 114	S68° 55' 06"W	60.05'
114 - 115	S68° 55' 06"W	62.47'
115 - 116	S40° 55' 43"W	168.47'
116 - 117	S40° 55' 43"W	60.00'
117 - 100	N49° 17' 48"W	32.82'

FOUND MONUMENT TABLE			
POINT	Y COORDS	X COORDS	DESCRIPTION
IP111	218286.664	375874.313	3/4" REBAR WITH YELLOW CAP
IP180	218287.125	375853.491	2" PIPE

UTILITY INTERESTS REQUIRED		
UTILITY NUMBER	OWNER (S)	INTEREST REQUIRED
200	CENTURYLINK	RELEASE OF RIGHTS

SCHEDULE OF LANDS & INTERESTS REQUIRED			
PARCEL NUMBER	OWNER (S)	INTERESTS REQUIRED	TLE SF
1	TINA M. SIDIE	TLE	136
2	VILLAGE OF MT STERLING	TLE	173
3	QUAMME APARTMENTS, LLC	TLE	184
4	JAMES A. WEDEBERG & JULIE A. WEDEBERG	TLE	428

NOTE: OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE WISCONSIN DEPARTMENT OF TRANSPORTATION



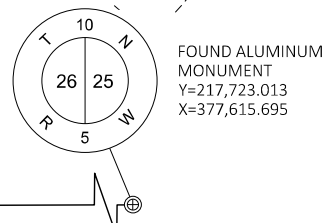
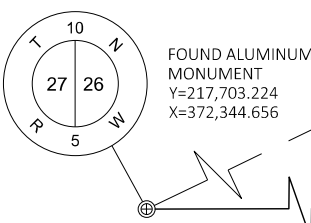
ENGINEERING, INC
Consultant Services

I, JAMES R. CAPPEART, PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF THE DEPARTMENT OF TRANSPORTATION, I HAVE SURVEYED AND MAPPED THIS TRANSPORTATION PROJECT PLAT AND SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.

SIGNATURE: *James Cappeart* DATE: 11/29/2021
PRINT NAME: JAMES R. CAPPEART
REGISTRATION NUMBER: S-3044

SIGNATURE: *Corey Schlager* DATE: 11/29/2021
PRINT NAME: COREY SCHLAGEL

THIS PLAT AND RELOCATION ORDER ARE APPROVED FOR THE WISCONSIN DEPARTMENT OF TRANSPORTATION SW REGION - MADISON



TRANSPORTATION PROJECT PLAT NO: 5540-02-21 - 4.02

THAT PART OF LOT 1 IN BLOCK 5 AND PART OF LOT 4 IN BLOCK 6 IN ORIGINAL PLAT OF MOUNT STERLING, BEING IN THE SOUTHWEST 1/4 OF THE NORTHEAST 1/4, ALL IN SECTION 26, TOWN 10 NORTH, RANGE 5 WEST, VILLAGE OF MOUNT STERLING, CRAWFORD COUNTY, WISCONSIN.

TO PROPERTY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE. THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE PROJECT.

TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SECTIONS 84.02 (3), 84.09, AND 84.30, WISCONSIN STATUTES, THE DEPARTMENT OF TRANSPORTATION HEREBY ORDERS THAT:

1. THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT'S LAD OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE PROJECT.

2. THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE DEPARTMENT FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE STATE OF WISCONSIN, PURSUANT TO THE PROVISIONS OF SUBSECTION 84.09 (1) OR (2), WISCONSIN STATUTES.

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), CRAWFORD COUNTY, MADISON (2011) IN US SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE:

EXISTING HIGHWAY RIGHT-OF-WAY FOR STH 27/MOUNT ST ESTABLISHED FROM PREVIOUS PROJECT 5543-01-23, 5545-02-21, ORIGINAL PLAT OF MOUNT STERLING.

EXISTING HIGHWAY RIGHT-OF-WAY FOR THIRD ST ESTABLISHED FROM PREVIOUS PROJECT 5543-01-23, 5545-02-21, AND ORIGINAL PLAT OF MOUNT STERLING.

EXISTING HIGHWAY RIGHT-OF-WAY FOR FOURTH ST ESTABLISHED FROM EXISTING CENTERLINE WITH A WIDTH OF 60' PER ORIGINAL PLAT OF MOUNT STERLING.

FOR THE LATEST ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN LA CROSSE.

NGS CONTROL POINT UTILIZED: PID DHS354, DESIGNATION: MOUNT STERLING GPS, CRAWFORD COUNTY, WI.

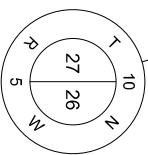
REFER TO TITLE SHEET, RECORDED AS SHEET 2 OF 2 OF DOCUMENT NO. 347206 FOR ADDITIONAL INFORMATION

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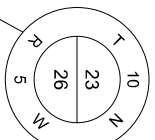
COURSE TABLE with columns: COURSE, BEARING, DISTANCE. Rows include courses 108 through 109.

UTILITY INTERESTS REQUIRED table with columns: UTILITY NUMBER, OWNER (S), INTEREST REQUIRED, RELEASE OF RIGHTS.

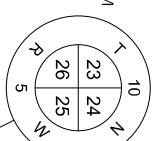
COMMONWEALTH TELEPHONE V.160, P.128, DOC. 117991 - PARCEL 5 BLANKET



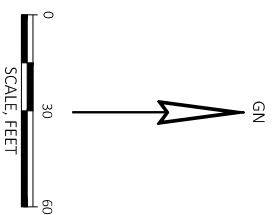
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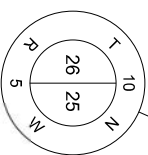
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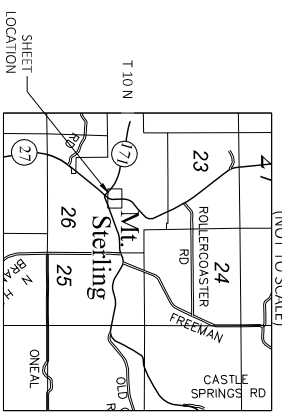
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RECORDED 12/13/2021 09:42 AM REGISTER OF DEEDS OFFICE CRAWFORD COUNTY, WI 25.00 RECORDED FEE PAGES: 1 VOLUME: 3PL PAGE: 127 *The above recording information verifies that this document has been electronically recorded and returned to the submitter.*



FOUND ALUMINUM MONUMENT Y=217,723.013 X=377,615.695



QUARTER LINE N89°47'06"E 5271.08' COR TO COR

VACATED ALLEY DEED P362 V249 DOC#245493

N9°10'04"E 353.94' COR TO TRAVEL POINT

N89°47'06"E 5271.08' COR TO COR

QUARTER LINE

QUARTER LINE

R 5 W R 4 W

SCHEDULE OF LANDS & INTERESTS REQUIRED

Table with columns: PARCEL NUMBER, OWNER (S), INTERESTS REQUIRED, FEE RW SF, REQUIRED NEW, EXISTING, TOTAL, TILE SF. Rows include parcels 5 and 6.

NOTE: OWNERS NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE WISCONSIN DEPARTMENT OF TRANSPORTATION

Professional seal for James R. Cappert, Wisconsin Land Surveyor, and signature block with date 12/13/2021.

TRANSPORTATION PROJECT PLAT NO: 5540-02-21 - 4.02 - AMENDMENT NO. 1

AMENDS TRANSPORTATION PROJECT PLAT 5540-02-21 - 4.02 RECORDED AS DOCUMENT NO. 347321 IN VOL.UME 39L, PAGE 127, REMOVES UTILITY NUMBER 200 AND ADDS PARCEL 250.

THAT PART OF LOT 1 IN BLOCK 5 AND PART OF LOT 4 IN BLOCK 6 IN ORIGINAL PLAT OF MOUNT STERLING, BEING IN THE SOUTHWEST 1/4 OF THE NORTHEAST 1/4, ALL IN SECTION 26, TOWN 10 NORTH, RANGE 5 WEST, VILLAGE OF MOUNT STERLING, CRAWFORD COUNTY, WISCONSIN.

RELOCATION ORDER STH 27 PRAIRIE DU CHIEN - VIRGOQUA (0.1 MI N NORTH AVE TO CTH C) CRAWFORD COUNTY.

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE PROJECT.

TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SUBSECTIONS 94.02(3), 94.09, AND 94.30, WISCONSIN STATUTES, THE DEPARTMENT OF TRANSPORTATION HEREBY ORDERS THAT:

- 1. THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS LAY OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE PROJECT.
2. THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE RELINQUISHED BY THE DEPARTMENT FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE STATE OF WISCONSIN, PURSUANT TO THE PROVISIONS OF SUBSECTION 94.09 (1) OR (2), WISCONSIN STATUTES.

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), CRAWFORD COUNTY, MAD083 (2011) IN US SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE: EXISTING HIGHWAY RIGHT-OF-WAY FOR STH 27/MOUNT ST ESTABLISHED FROM PREVIOUS PROJECT 5543-01-23, 5545-02-21, ORIGINAL PLAT OF MOUNT STERLING.

EXISTING HIGHWAY RIGHT-OF-WAY FOR FOURTH ST ESTABLISHED FROM PREVIOUS PROJECT 5543-01-23, 5545-02-21, AND ORIGINAL PLAT OF MOUNT STERLING.

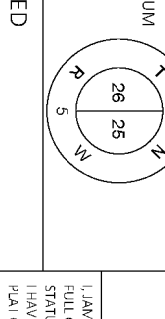
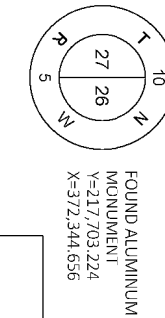
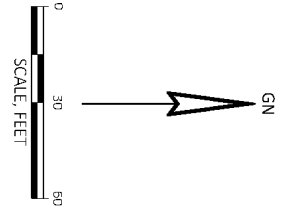
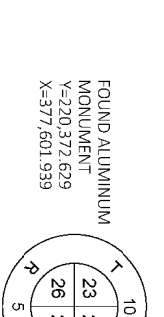
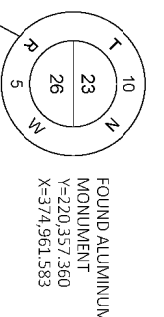
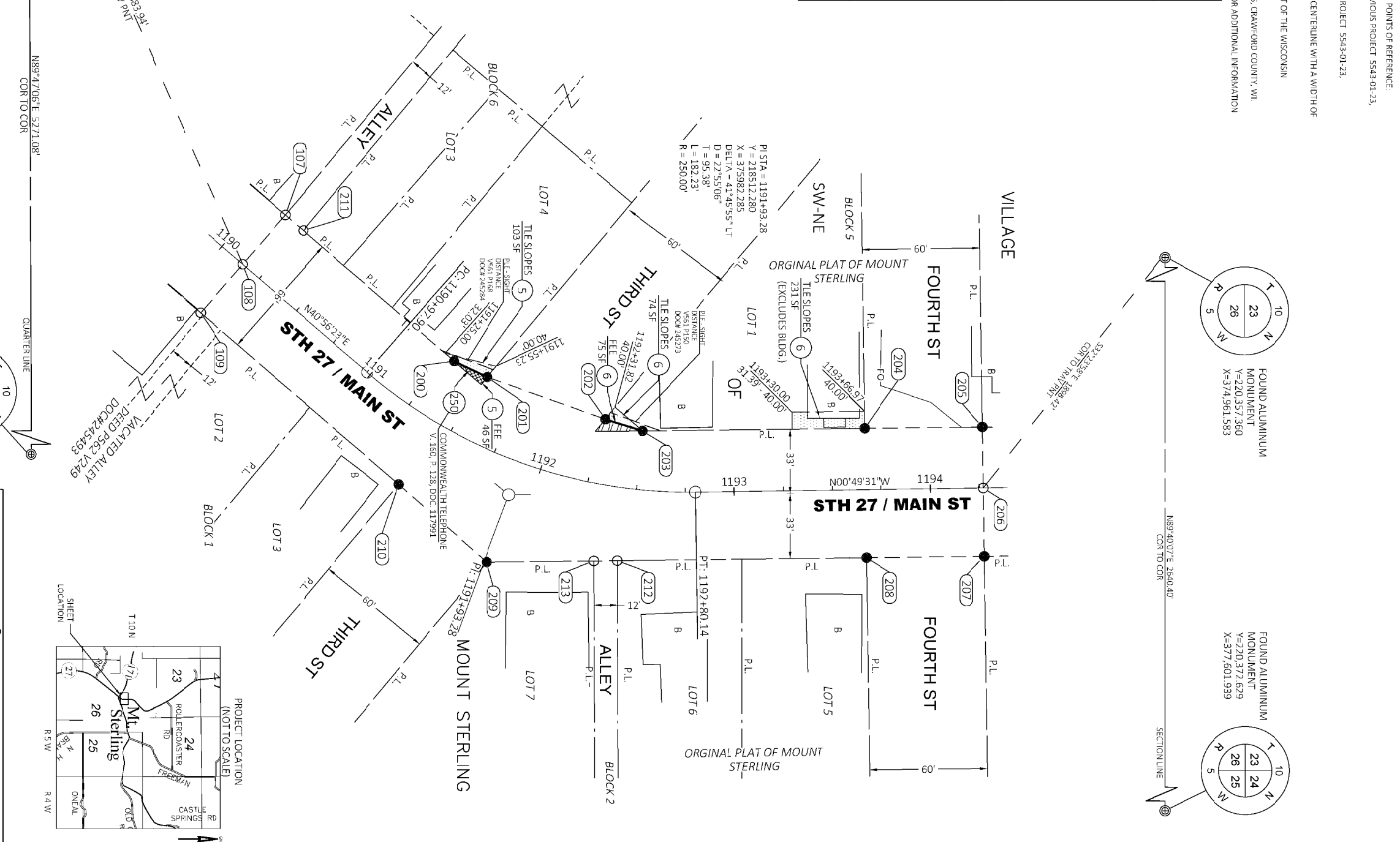
EXISTING HIGHWAY RIGHT-OF-WAY FOR THIRD ST ESTABLISHED FROM PREVIOUS PROJECT 5543-01-23, 5545-02-21, AND ORIGINAL PLAT OF MOUNT STERLING.

FOR THE LATEST ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN LA CROSSE.

NGS CONTROL POINT UTILIZED: PID DHE354, DESIGNATION: MOUNT STERLING GPS, CRAWFORD COUNTY, WI. REFER TO TITLE SHEET, RECORDED AS SHEET 2 OF 2 OF DOCUMENT NO. 347266 FOR ADDITIONAL INFORMATION.

RIGHT OF WAY POINT TABLE with columns: POINT, STATION, OFFSET, COORDS Y, COORDS X. Contains 13 points from 107 to 213.

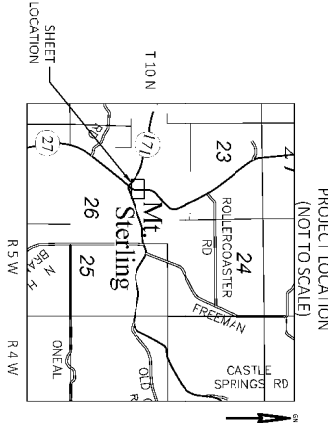
COURSE TABLE with columns: COURSE, BEARING, DISTANCE. Contains 13 courses from 108 to 109.



RESERVED FOR REGISTER OF DEEDS PROJECT NUMBER 5540-02-21-4.02 AMENDMENT NO. 1

RECORDED 03/17/2023 10:48 AM MELISSA C NAGEL REGISTER OF DEEDS OFFICE CRAWFORD COUNTY, WI 25.00

SCHEDULE OF LANDS & INTERESTS REQUIRED table with columns: PARCEL NUMBER, OWNER(S), INTERESTS REQUIRED, FEE R/W SF, FEE & TIE, NEW, EXISTING, TOTAL, TIE SF, EASEMENT SF.



NOTE: OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE WISCONSIN DEPARTMENT OF TRANSPORTATION.

Professional seal and signature block for James R. Cappert, Wisconsin Land Surveyor, dated 3/16/2023.

TRANSPORTATION PROJECT PLAT NO: 5540-02-21 - 4.03

THAT PART OF LOT 1 IN BLOCK 4 IN ORIGINAL PLAT OF MOUNT STERLING, BEING IN THE SOUTHWEST 1/4 OF THE NORTHEAST AND INCLUDING THAT PART OF THE NORTHWEST 1/4 OF THE NORTHEAST 1/4, ALL IN SECTION 26, TOWN 10 NORTH, RANGE 5 WEST, VILLAGE OF MOUNT STERLING, CRAWFORD COUNTY, WISCONSIN.

RELOCATION ORDER 5TH 27 PRAIRIE DU CHIEN - VIROQUA (0.1 MI N NORTH AVE TO CTH C) CRAWFORD COUNTY TO PROPERTY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE. THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE PROJECT. TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SUBSECTIONS 84.02(1), 84.09, AND 84.30, WISCONSIN STATUTES, THE DEPARTMENT OF TRANSPORTATION HEREBY ORDERS THAT:

1. THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS LAY OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE PROJECT.
2. THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE DEPARTMENT FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE STATE OF WISCONSIN, PURSUANT TO THE PROVISIONS OF SUBSECTION 84.09 (1) OR (2), WISCONSIN STATUTES.

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), CRAWFORD COUNTY, NAD83 (2011) IN US SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE:

EXISTING HIGHWAY RIGHT-OF-WAY FOR 5TH 27 MAIN ST ESTABLISHED FROM PREVIOUS PROJECT 5543-01-23, 5545-02-21, AND ORIGINAL PLAT OF MOUNT STERLING.

EXISTING HIGHWAY RIGHT-OF-WAY FOR FIFTH ST ESTABLISHED FROM PREVIOUS PROJECT 5543-01-23, AND ORIGINAL PLAT OF MOUNT STERLING.

SIXTEENTH LINE WAS ESTABLISHED AS NORTH RIGHT-OF-WAY OF FIFTH ST PER ORIGINAL PLAT OF MOUNT STERLING.

FOR THE LATEST ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN LA CROSSE.

NGS CONTROL POINT UTILIZED: PID D85354, DESIGNATION: MOUNT STERLING GPS, CRAWFORD COUNTY, WI.

REFER TO TITLE SHEET, RECORDED AS SHEET 2 OF 2 OF DOCUMENT NO. 347206 FOR ADDITIONAL INFORMATION.

SCHEDULE OF LANDS & INTERESTS REQUIRED					
PARCEL NUMBER	OWNER (S)	INTERESTS REQUIRED	FEE R/W SF, REQUIRED	TILE SF	
			NEW	EXISTING	TOTAL
7	THOMAS M & SANDRA K CODY	TILE			70
8	RONALD H & KATHERINE K QUAMME	TILE			468

NOTE: OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE WISCONSIN DEPARTMENT OF TRANSPORTATION

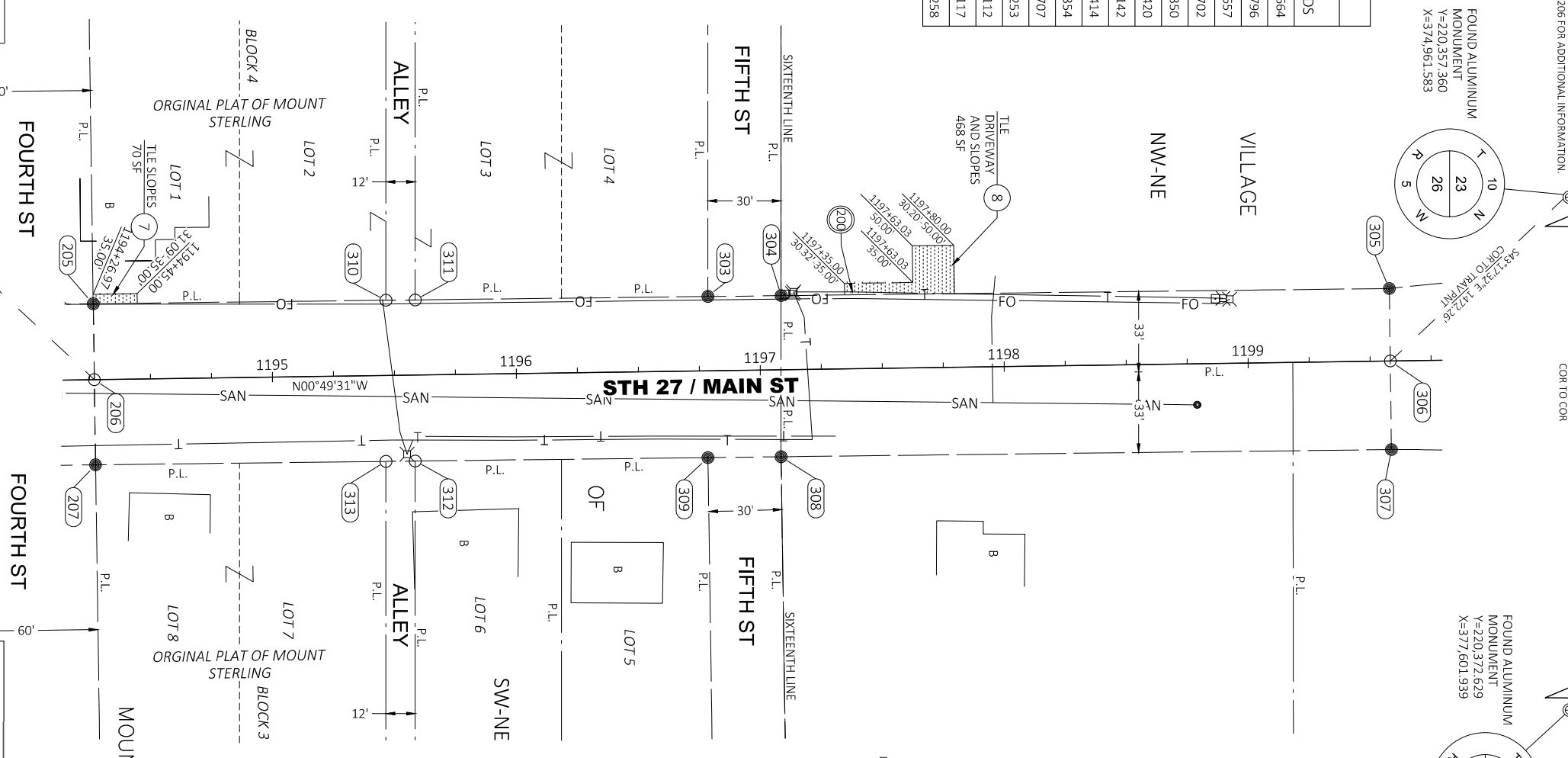
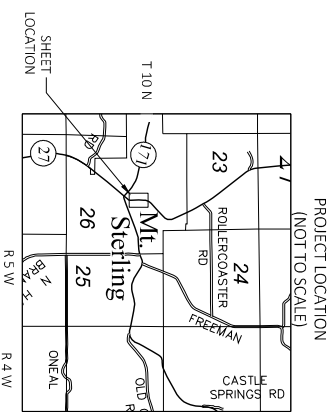
RESERVED FOR REGISTER OF DEEDS

PROJECT NUMBER 5540-02-21-4.03

RECORDED 12/13/2021 09:48 AM
 REGISTER OF DEEDS OFFICE
 CRAWFORD COUNTY, WI
 RECORDING FEE 25.00
 PAGES: 1
 VOLUME: 3PL PAGE: 128
 The above recording information verifies that this document has been electronically recorded and returned to the submitter.

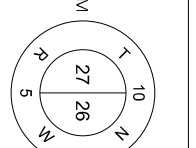
RIGHT OF WAY POINT TABLE			
POINT	STATION	OFFSET	COORDS
205	1194+26.97	31.14' LT	218754.011 375947.664
206	1194+26.97	0.00'	218754.459 375978.796
207	1194+26.97	34.86' RT	218754.962 376013.657
303	1196+78.98	30.47' LT	219006.011 375944.702
304	1197+08.98	30.39' LT	219036.011 375944.350
305	1199+58.32	29.73' LT	219285.328 375941.420
306	1199+58.32	0.00'	219285.756 375971.142
307	1199+58.22	36.27' RT	219286.182 376007.414
308	1197+08.03	35.61' RT	219036.011 376010.354
309	1196+78.03	35.53' RT	219006.013 376010.707
310	1195+46.97	30.82' LT	218874.011 375946.253
311	1195+58.97	30.79' LT	218886.011 375946.112
312	1195+58.02	35.21' RT	218886.011 376012.117
313	1195+46.02	35.18' RT	218874.011 376012.258

COURSE TABLE		
COURSE	BEARING	DISTANCE
206 - 205	S89° 10' 29" W	31.14'
205 - 310	N00° 40' 24" W	120.01'
310 - 311	N00° 40' 24" W	12.00'
311 - 303	N00° 40' 24" W	120.01'
303 - 304	N00° 40' 24" W	30.00'
304 - 305	N00° 40' 24" W	249.33'
305 - 306	N89° 10' 29" E	29.73'
306 - 307	N89° 19' 36" E	36.27'
307 - 308	S00° 40' 24" E	250.19'
308 - 309	S00° 40' 24" E	30.00'
309 - 312	S00° 40' 24" E	120.01'
312 - 313	S00° 40' 24" E	12.00'
313 - 207	S00° 40' 24" E	119.06'
207 - 206	S89° 10' 29" W	34.86'

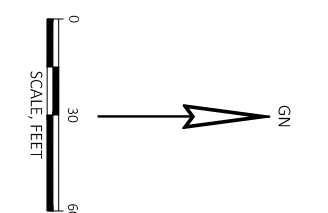
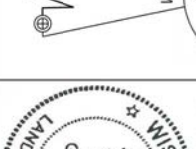


UTILITY INTERESTS REQUIRED		
UTILITY NUMBER	OWNER (S)	INTEREST REQUIRED
200	CENTURYLINK	RELEASE OF RIGHTS
202	VALLEY RIDGE CLEAN WATER COMMISSION	RELEASE OF RIGHTS

202 VALLEY RIDGE CLEAN WATER COMMISSION
 V. 548, P. 44, DOC. 242354 - PARCEL 7 BLANKET
 200 CENTURYLINK
 FOUND ALUMINUM MONUMENT
 Y=217,703.224 X=372,344.656



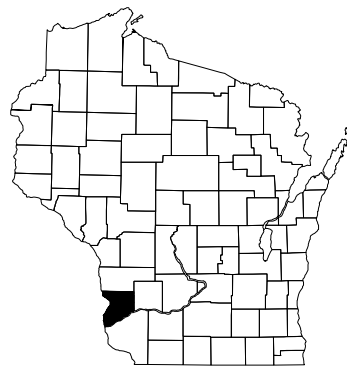
FOUND ALUMINUM MONUMENT
 Y=217,723.013 X=377,615.695



I, JAMES R. CAPPEART, PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF THE DEPARTMENT OF TRANSPORTATION, I HAVE SURVEYED AND MAPPED THIS TRANSPORTATION PROJECT PLAT AND SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED AND...

ENGINEERING, INC
 CONSULTING SERVICES
 SIGNATURE: *James R. Cappeart* DATE: 12/13/2021
 PRINT NAME: JAMES R. CAPPEART
 REGISTRATION NUMBER: S-3044
 THIS PLAT AND RELOCATION ORDER ARE APPROVED FOR THE WISCONSIN DEPARTMENT OF TRANSPORTATION SW REGION - MADISON
 SIGNATURE: *Corey Schlegel* DATE: 12/13/2021
 PRINT NAME: COREY SCHLEGEL

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION TRANSPORTATION PROJECT PLAT TITLE SHEET 5540-02-21 PRAIRIE DU CHIEN - VIROQUA 0.1 MI N NORTH AVE TO CTH C



CONVENTIONAL SYMBOLS

SECTION LINE		SECTION CORNER SYMBOL		R/W MONUMENT (TO BE SET)	
QUARTER LINE		SECTION CORNER MONUMENT		NON-MONUMENTED R/W POINT	
SIXTEENTH LINE		GEODETIC SURVEY MONUMENT		FOUND IRON PIN (1-INCH UNLESS NOTED)	
NEW REFERENCE LINE		SIGN		OFF-PREMISE SIGN	
NEW R/W LINE		ELECTRIC POLE		COMPENSABLE	
EXISTING R/W OR HE LINE		TELEPHONE POLE		NON-COMPENSABLE	
PROPERTY LINE		PEDESTAL (LABEL TYPE) (TV, TEL, ELEC, ETC.)			
LOT, TIE & OTHER MINOR LINES		PULLBOX (LABEL TYPE) (TV, TEL, ELEC, ETC.)			
SLOPE INTERCEPT		ACCESS RESTRICTED BY ACQUISITION			
CORPORATE LIMITS		NO ACCESS (BY STATUTORY AUTHORITY)			
UNDERGROUND FACILITY (COMMUNICATIONS, ELECTRIC, ETC.)		ACCESS RESTRICTED (BY PREVIOUS PROJECT OR CONTROL)			
NEW R/W (FEE OR HE) (HATCHING VARIES BY OWNER)		NO ACCESS (NEW HIGHWAY)			
TEMPORARY LIMITED EASEMENT AREA		PARCEL NUMBER (25)		UTILITY NUMBER (40)	
EASEMENT AREA (PERMANENT LIMITED OR RESTRICTED DEVELOPMENT)					
TRANSMISSION STRUCTURES					
BUILDING TO BE REMOVED					
BRIDGE					
CULVERT					
		PARALLEL OFFSETS			

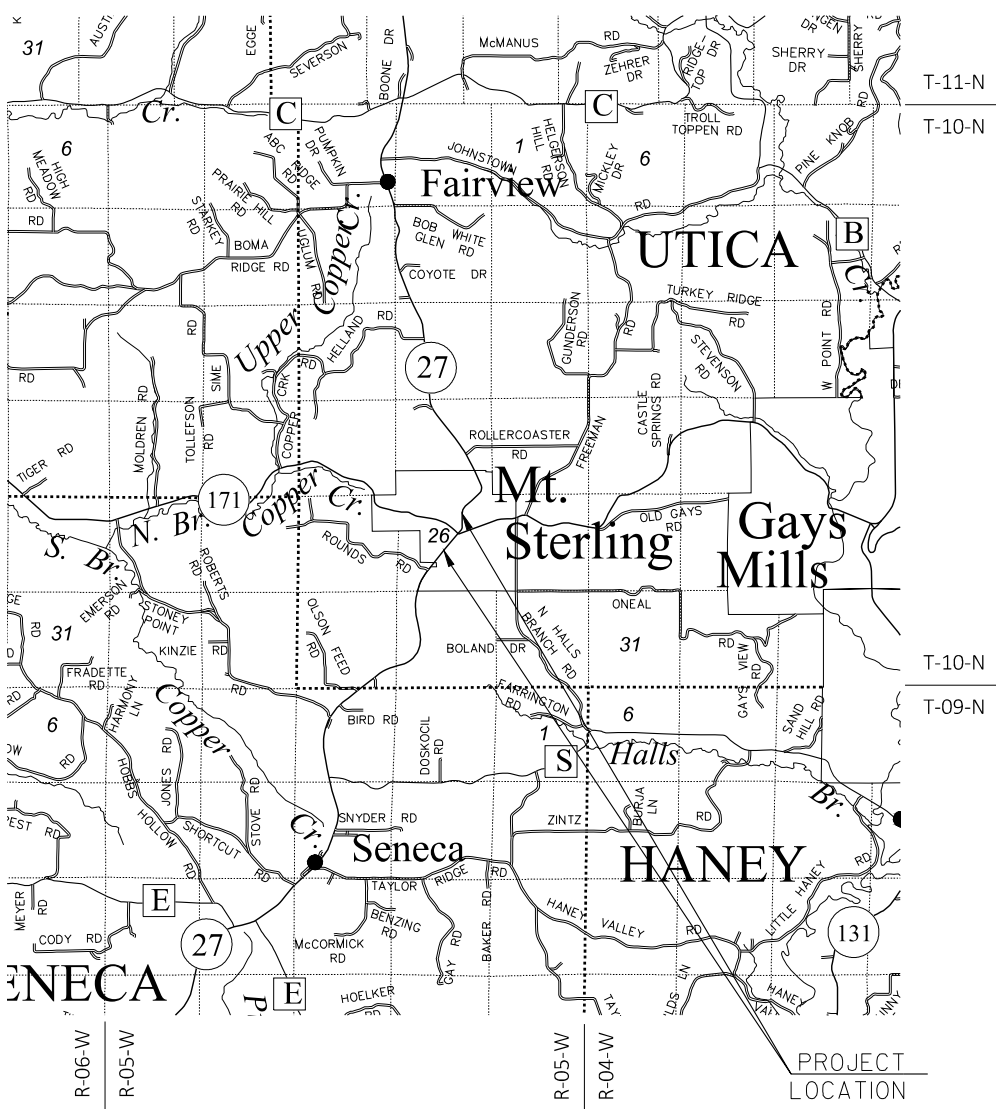
ACCESS RIGHTS	AR	POINT OF INTERSECTION	PI
ACRES	AC	PROPERTY LINE	PL
AHEAD	AH	RECORDED AS	(100')
ALUMINUM	ALUM	REEL / IMAGE	R/I
AND OTHERS	ET AL	REFERENCE LINE	R/L
BACK	BK	REMAINING	REM
BLOCK	BLK	RESTRICTIVE DEVELOPMENT	RDE
CENTERLINE	C/L	EASEMENT	
CERTIFIED SURVEY MAP	CSM	RIGHT	RT
CONCRETE	CONC	RIGHT OF WAY	R/W
COUNTY	CO	SECTION	SEC
COUNTY TRUNK HIGHWAY	CTH	SEPTIC VENT	SEPV
DISTANCE	DIST	SQUARE FEET	SF
CORNER	COR	STATE TRUNK HIGHWAY	STH
DOCUMENT NUMBER	DOC	STATION	STA
EASEMENT	EASE	TELEPHONE PEDESTAL	TP
EXISTING	EX	TEMPORARY LIMITED EASEMENT	TLE
GAS VALVE	GV		
GRID NORTH	GN	TRANSPORTATION PROJECT PLAT	TPP
HIGHWAY EASEMENT	HE		
IDENTIFICATION	ID	UNITED STATES HIGHWAY	USH
LAND CONTRACT	LC	VOLUME	V
LEFT	LT		
MONUMENT	MON		
NATIONAL GEODETIC SURVEY NUMBER	NGS		
OUTLOT	OL		
PAGE	P		
POINT OF TANGENCY	PT		
PERMANENT LIMITED EASEMENT	PLE		
POINT OF BEGINNING	POB		
POINT OF CURVATURE	PC		
POINT OF COMPOUND CURVE	PCC		

CURVE DATA

LONG CHORD	LCH
LONG CHORD BEARING	LCB
RADIUS	R
DEGREE OF CURVE	D
CENTRAL ANGLE	Δ/DELTA
LENGTH OF CURVE	L
TANGENT	T
DIRECTION AHEAD	DA
DIRECTION BACK	DB

CONVENTIONAL UTILITY SYMBOLS

	WATER
	GAS
	TELEPHONE OVERHEAD
	TRANSMISSION LINES
	ELECTRIC
	CABLE TELEVISION
	FIBER OPTIC
	SANITARY SEWER
	STORM SEWER



THE NOTES, CONVENTIONAL SIGNS, AND ABBREVIATIONS ARE ASSOCIATED WITH EACH TRANSPORTATION PROJECT PLAT FOR PROJECT 5540-02-21

NOTES:
POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), CRAWFORD COUNTY, NAD83 (2011) IN US SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.
ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 3/4" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.
ALL RIGHT-OF-WAY LINES DEPICTED IN THE NON-ACQUISITION AREAS ARE INTENDED TO RE-ESTABLISH EXISTING RIGHT-OF-WAY LINES AS DETERMINED FROM PREVIOUS PROJECTS, OTHER RECORDED DOCUMENTS, OR FROM CENTERLINE OF EXISTING PAVEMENTS.
RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS" OF PUBLIC RECORD.
DIMENSIONING FOR THE NEW RIGHT-OF-WAY IS MEASURED ALONG AND PERPENDICULAR TO THE NEW REFERENCE LINES.
A TEMPORARY LIMITED EASEMENT (TLE) IS A RIGHT FOR CONSTRUCTION PURPOSES, AS DEFINED HEREIN, INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON, THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM DESIRABLE. ALL (TLEs) ON THIS PLAT EXPIRE AT THE COMPLETION OF THE CONSTRUCTION PROJECT FOR WHICH THIS INSTRUMENT IS GIVEN.
A PERMANENT LIMITED EASEMENT (PLE) IS A RIGHT FOR CONSTRUCTION AND MAINTENANCE PURPOSES, AS DEFINED HEREIN, INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON AND THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM DESIRABLE, BUT WITHOUT PREJUDICE TO THE OWNER'S RIGHTS TO MAKE OR CONSTRUCT IMPROVEMENTS ON SAID LANDS OR TO FLATTEN THE SLOPES, PROVIDING SAID ACTIVITIES WILL NOT IMPAIR OR OTHERWISE ADVERSELY AFFECT THE HIGHWAY FACILITIES.
PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM DATA DERIVED FROM MAPS AND DOCUMENTS OF PUBLIC RECORD AND/OR EXISTING OCCUPATIONAL LINES. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.
FOR THE LATEST ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN LACROSSE.
PARCEL IDENTIFICATION NUMBERS MAY NOT POINT TO ALL AREAS OF ACQUISITION, AS NOTED ON THE SCHEDULE OF LANDS & INTERESTS REQUIRED.
INFORMATION FOR THE BASIS OF EXISTING HIGHWAY RIGHT-OF-WAY POINTS OF REFERENCE AND ACCESS CONTROL ARE LISTED ON THE TPP DETAIL PAGES.

PROJECT NUMBER 5540-02-21 - 4.01
SHEET 2 OF 2

RESERVED FOR REGISTER OF DEEDS
PROJECT NUMBER 5540-02-21-4.01
SHEET 1 OF 2

TRANSPORTATION PROJECT PLAT NO: 5540-02-21 - 4.01

THAT PART OF LOT 1 IN BLOCK 1, AND PART OF LOT 1 AND LOT 2 IN BLOCK 6, AND PART OF LOT 1 IN BLOCK 7, AND THAT PART OF VACATED SECOND STREET, ALL IN THE ORIGINAL PLAT OF MOUNT STERLING, BEING IN AND INCLUDING PART OF THE SOUTHWEST 1/4 OF THE NORTHEAST 1/4, ALL IN SECTION 26, TOWN 10 NORTH, RANGE 5 WEST, VILLAGE OF MOUNT STERLING, CRAWFORD COUNTY, WISCONSIN.

RELOCATION ORDER STH 27 PRAIRIE DU CHIEN - VIROQUA (0.1 MI N NORTH AVE TO CTH C) CRAWFORD COUNTY

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE PROJECT.

TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SUBSECTIONS 84.02 (3), 84.09, AND 84.30, WISCONSIN STATUTES, THE DEPARTMENT OF TRANSPORTATION HEREBY ORDERS THAT:

1. THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS LAID OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE PROJECT.
2. THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE DEPARTMENT FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE STATE OF WISCONSIN, PURSUANT TO THE PROVISIONS OF SUBSECTION 84.09 (1) OR (2), WISCONSIN STATUTES.

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), CRAWFORD COUNTY, NAD83 (2011) IN US SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE:

EXISTING HIGHWAY RIGHT-OF-WAY FOR STH 27/MAIN ST ESTABLISHED FROM PREVIOUS PROJECT 5543-01-23, 5545-02-21, TO380(6)

EXISTING HIGHWAY RIGHT-OF-WAY FOR FIRST ST ESTABLISHED FROM CSM 100(NOT SHOWN IN DETAIL), CSM 775(NOT SHOWN IN DETAIL), AND PLAT OF SURVEY 1971 (RAHA)(NOT SHOWN IN DETAIL), ORIGINAL PLAT OF MOUNT STERLING.

EXISTING HIGHWAY RIGHT-OF-WAY FOR STH 171/SECOND ST ESTABLISHED FROM PREVIOUS PROJECT 5543-01-23, 5545-02-21, TO380(6), DJ-9279A, CSM 35 DOC 165283 (NOT SHOWN IN DETAIL), ORIGINAL PLAT OF MOUNT STERLING.

EXISTING HIGHWAY RIGHT-OF-WAY FOR SECOND ST ESTABLISHED FROM EXISTING CENTERLINE WITH A WIDTH OF 60' PER ORIGINAL PLAT OF MOUNT STERLING.

EXISTING HIGHWAY RIGHT-OF-WAY FOR STH 171/DIAGONAL ST ESTABLISHED FROM PREVIOUS PROJECT 5543-01-23, PLAT OF SURVEY 2013 (WARNER)(NOT SHOWN IN DETAIL), ORIGINAL PLAT OF MOUNT STERLING.

FOR THE LATEST ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN LA CROSSE.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 3/4" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

NGS CONTROL POINT UTILIZED: PID DHS534, DESIGNATION: MOUNT STERLING GPS, CRAWFORD COUNTY, WI.

REFER TO TITLE SHEET, RECORDED AS SHEET 2 OF 2 OF THIS DOCUMENT FOR ADDITIONAL INFORMATION.

RIGHT OF WAY POINT TABLE				
POINT	STATION	OFFSET	Y COORDS	X COORDS
100	1185+19.74	0.00'	218003.487	375540.936
101	1185+19.60	33.18' LT	218025.127	375515.780
102	1185+79.60	33.19' LT	218070.459	375555.088
103	1187+74.72	33.23' LT	218217.877	375682.914
104	1187+75.42	58.42' LT	218234.907	375664.347
105	1188+79.38	55.53' LT	218311.549	375734.647
106	1188+91.27	33.26' LT	218305.930	375759.264
107	1190+13.88	33.28' LT	218398.569	375839.591
108	1190+13.89	0.00'	218376.766	375864.735
109	1190+13.90	32.72' RT	218355.331	375889.456
110	1189+52.13	32.73' RT	218308.664	375848.991
IP111	1189+52.10	66.28' RT	218286.664	375874.313
112	1189+21.14	124.56' RT	218225.081	375898.053
113	1188+56.54	90.25' RT	218198.772	375829.806
114	1188+03.52	62.08' RT	218177.174	375773.779
115	1187+48.35	32.77' RT	218154.704	375715.493
116	1185+79.88	32.81' RT	218027.417	375605.122
117	1185+19.88	32.82' RT	217982.085	375565.815

COURSE TABLE		
COURSE	BEARING	DISTANCE
100 - 101	N49° 17' 48"W	33.18'
101 - 102	N40° 55' 43"E	60.00'
102 - 103	N40° 55' 43"E	195.12'
103 - 104	N47° 28' 17"W	25.19'
104 - 105	N42° 31' 43"E	104.00'
105 - 106	S77° 08' 37"E	25.25'
106 - 107	N40° 55' 43"E	122.61'
107 - 108	S49° 04' 17"E	33.28'
108 - 109	S49° 04' 17"E	32.72'
109 - 110	S40° 55' 43"W	61.77'
110 - IP111	S49° 00' 59"E	33.54'
IP111 - 112	S21° 04' 54"E	66.00'
112 - 113	S68° 55' 06"W	73.14'
113 - 114	S68° 55' 06"W	60.05'
114 - 115	S68° 55' 06"W	62.47'
115 - 116	S40° 55' 43"W	168.47'
116 - 117	S40° 55' 43"W	60.00'
117 - 100	N49° 17' 48"W	32.82'

FOUND MONUMENT TABLE			
POINT	Y COORDS	X COORDS	DESCRIPTION
IP111	218286.664	375874.313	3/4" REBAR WITH YELLOW CAP
IP180	218287.125	375853.491	2" PIPE

UTILITY INTERESTS REQUIRED		
UTILITY NUMBER	OWNER (S)	INTEREST REQUIRED
200	CENTURYLINK	RELEASE OF RIGHTS

SCHEDULE OF LANDS & INTERESTS REQUIRED			
PARCEL NUMBER	OWNER (S)	INTERESTS REQUIRED	TLE SF
1	TINA M. SIDIE	TLE	136
2	VILLAGE OF MT STERLING	TLE	173
3	QUAMME APARTMENTS, LLC	TLE	184
4	JAMES A. WEDEBERG & JULIE A. WEDEBERG	TLE	428

NOTE: OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE WISCONSIN DEPARTMENT OF TRANSPORTATION

ENGINEERING, INC.
Consultant Services

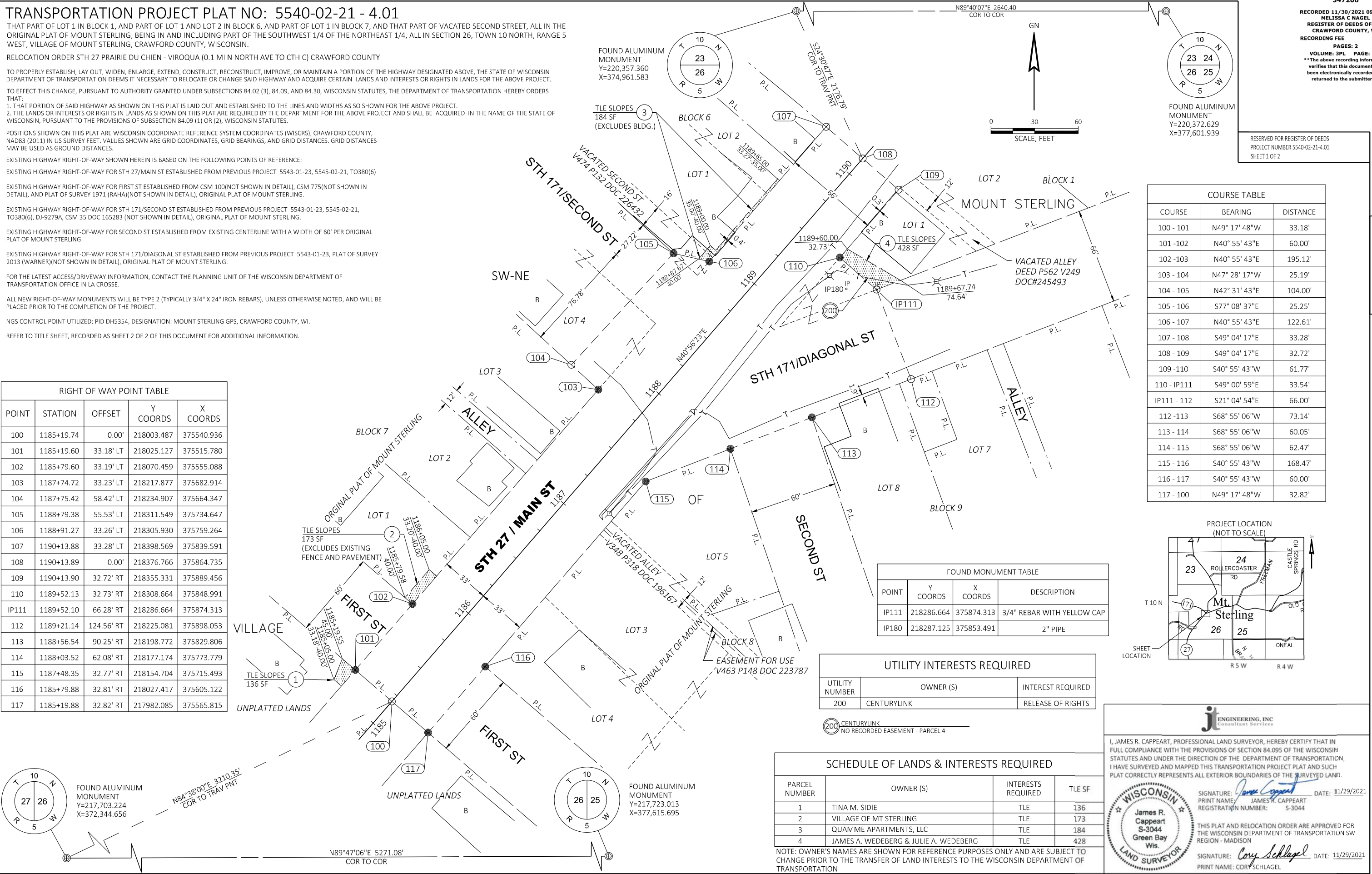
I, JAMES R. CAPPEART, PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF THE DEPARTMENT OF TRANSPORTATION, I HAVE SURVEYED AND MAPPED THIS TRANSPORTATION PROJECT PLAT AND SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.

SIGNATURE: *James Cappeart* DATE: 11/29/2021
PRINT NAME: JAMES R. CAPPEART
REGISTRATION NUMBER: S-3044

James R. Cappeart
S-3044
Green Bay Wis.
LAND SURVEYOR

THIS PLAT AND RELOCATION ORDER ARE APPROVED FOR THE WISCONSIN DEPARTMENT OF TRANSPORTATION SW REGION - MADISON

SIGNATURE: *Corey Schlagel* DATE: 11/29/2021
PRINT NAME: COREY SCHLAGEL



4

4

TRANSPORTATION PROJECT PLAT NO: 5540-02-21 - 4.02

THAT PART OF LOT 1 IN BLOCK 5 AND PART OF LOT 4 IN BLOCK 6 IN ORIGINAL PLAT OF MOUNT STERLING, BEING IN THE SOUTHWEST 1/4 OF THE NORTHEAST 1/4, ALL IN SECTION 26, TOWN 10 NORTH, RANGE 5 WEST, VILLAGE OF MOUNT STERLING, CRAWFORD COUNTY, WISCONSIN.

RELOCATION ORDER STH 27 PRAIRIE DU CHIEN - VIRROQUA (0.1 MI N NORTH AVE TO CTH C) CRAWFORD COUNTY.

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE PROJECT.

TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SUBSECTIONS 84.02 (3), 84.08, AND 84.30, WISCONSIN STATUTES, THE DEPARTMENT OF TRANSPORTATION HEREBY ORDERS THAT:

- 1. THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS LAID OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE PROJECT.
2. THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE ACQUIRED BY THE DEPARTMENT FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE STATE OF WISCONSIN, PURSUANT TO THE PROVISIONS OF SUBSECTION 84.09 (1) OR (2), WISCONSIN STATUTES.

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), CRAWFORD COUNTY, NAD83 (2011) IN US SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE:
EXISTING HIGHWAY RIGHT-OF-WAY FOR STH 27 MAIN ST ESTABLISHED FROM PREVIOUS PROJECT 5543-01-23, 5545-02-21, ORIGINAL PLAT OF MOUNT STERLING.

EXISTING HIGHWAY RIGHT-OF-WAY FOR THIRD ST ESTABLISHED FROM PREVIOUS PROJECT 5543-01-23, 5545-02-21, AND ORIGINAL PLAT OF MOUNT STERLING.

EXISTING HIGHWAY RIGHT-OF-WAY FOR FOURTH ST ESTABLISHED FROM EXISTING CENTERLINE WITH A WIDTH OF 60' PER ORIGINAL PLAT OF MOUNT STERLING.

FOR THE LATEST ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN LA CROSSE.

NGS CONTROL POINT UTILIZED: PID DMS354, DESIGNATION: MOUNT STERLING GPS, CRAWFORD COUNTY, WI. REFER TO TITLE SHEET, RECORDED AS SHEET 2 OF 2 OF DOCUMENT NO. 347206 FOR ADDITIONAL INFORMATION.

RIGHT OF WAY POINT TABLE table with columns: POINT, STATION, OFFSET, Y COORDS, X COORDS. Rows 107-213.

COURSE TABLE table with columns: COURSE, BEARING, DISTANCE. Rows 108-109, 211-210, 200-201, etc.

UTILITY INTERESTS REQUIRED table with columns: UTILITY NUMBER, OWNER(S), INTEREST REQUIRED. Row 200 CENTURYLINK.

COMMONWEALTH TELEPHONE V. 160, P. 178, DOC. 117991 - PARCEL 5 BLANKET

FOUND ALUMINUM MONUMENT Y=217,703.224 X=372,344.656

FOUND ALUMINUM MONUMENT Y=220,372.639 X=377,601.939

SCHEDULE OF LANDS & INTERESTS REQUIRED table with columns: PARCEL NUMBER, OWNER(S), INTERESTS REQUIRED, FEE R/W REQUIRED, TLE SF.

NOTE: OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE WISCONSIN DEPARTMENT OF TRANSPORTATION.

347321 RECORDED 12/14/2021 09:42 AM MELISSA C. HAGER REGISTER OF DEEDS OFFICE CRAWFORD COUNTY, WI 25 5540-02-21-4 PAGES: 1 VOLUME: 391 PAGE: 127

RESERVED FOR REGISTER OF DEEDS PROJECT NUMBER 5540-02-21-4.02

1 IN 30 FT PLOT SCALE: 11/30/21

JAMES CAPPEART PLOT BY: 12/13/2021 1:34 PM

PLOT DATE: 12/13/2021 1:34 PM

PLOT NAME: James Cappeart

PLOT BR: 12/13/2021 1:34 PM

PLOT DATE: 12/13/2021 1:34 PM

PLOT DATE: 12/13/2021 1:34 PM

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RECORDED 12/14/2021 09:45 AM
MELISSA C HAGEM
REGISTER OF DEEDS OFFICE
CRAWFORD COUNTY, WI
RECORDING FEE \$25.00
PAGES: 1
VOLUME: 3PL PAGE: 128
This document has been electronically recorded and returned to the submitter.

TRANSPORTATION PROJECT PLAT NO: 5540-02-21 - 4.03

THAT PART OF LOT 1 IN BLOCK 4 IN ORIGINAL PLAT OF MOUNT STERLING, BEING IN THE SOUTHWEST 1/4 OF THE NORTHEAST AND INCLUDING THAT PART OF THE NORTHWEST 1/4 OF THE NORTHEAST 1/4, ALL IN SECTION 26, TOWN 10 NORTH, RANGE 5 WEST, VILLAGE OF MOUNT STERLING, CRAWFORD COUNTY, WISCONSIN.

RELOCATION ORDER STH 27 PRAIRIE DU CHIEN - VIROQUA (0.1 MI N NORTH AVE TO CTH C) CRAWFORD COUNTY TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE PROJECT.

TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SUBSECTIONS 84.02 (3), 84.09, AND 84.30, WISCONSIN STATUTES, THE DEPARTMENT OF TRANSPORTATION HEREBY ORDERS THAT:
1. THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS LAID OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE PROJECT.
2. THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE DEPARTMENT FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE STATE OF WISCONSIN, PURSUANT TO THE PROVISIONS OF SUBSECTION 84.09 (1) OR (2), WISCONSIN STATUTES.

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), CRAWFORD COUNTY, NAD83 (2011) IN US SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE:

EXISTING HIGHWAY RIGHT-OF-WAY FOR STH 27/MAIN ST ESTABLISHED FROM PREVIOUS PROJECT 5543-01-23, 5545-02-21, AND ORIGINAL PLAT OF MOUNT STERLING.

EXISTING HIGHWAY RIGHT-OF-WAY FOR FOURTH ST ESTABLISHED FROM EXISTING CENTERLINE WITH A WIDTH OF 60' PER ORIGINAL PLAT OF MOUNT STERLING.

EXISTING HIGHWAY RIGHT-OF-WAY FOR FIFTH ST ESTABLISHED FROM PREVIOUS PROJECT 5543-01-23, AND ORIGINAL PLAT OF MOUNT STERLING.

SIXTEENTH LINE WAS ESTABLISHED AS NORTH RIGHT-OF-WAY OF FIFTH ST PER ORIGINAL PLAT OF MOUNT STERLING.

FOR THE LATEST ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN LA CROSSE.

NGS CONTROL POINT UTILIZED: PID DHE354, DESIGNATION: MOUNT STERLING GPS, CRAWFORD COUNTY, WI.

REFER TO TITLE SHEET, RECORDED AS SHEET 2 OF 2 OF DOCUMENT NO. 347206 FOR ADDITIONAL INFORMATION.

RESERVED FOR REGISTER OF DEEDS
PROJECT NUMBER 5540-02-21-4.03

SCHEDULE OF LANDS & INTERESTS REQUIRED						
PARCEL NUMBER	OWNER (S)	INTERESTS REQUIRED	FEE R/W SF, REQUIRED			TLE SF
			NEW	EXISTING	TOTAL	
7	THOMAS M & SANDRA K CODY	TLE	---	---	---	70
8	RONALD H & KATHERINE K QUAMME	TLE	---	---	---	468

NOTE: OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE WISCONSIN DEPARTMENT OF TRANSPORTATION

RIGHT OF WAY POINT TABLE

POINT	STATION	OFFSET	Y COORDS	X COORDS
205	1194+26.97	31.14' LT	218754.011	375947.664
206	1194+26.97	0.00'	218754.459	375978.796
207	1194+26.97	34.86' RT	218754.962	376013.657
303	1196+78.98	30.47' LT	219006.011	375944.702
304	1197+08.98	30.39' LT	219036.011	375944.350
305	1199+58.32	29.73' LT	219285.328	375941.420
306	1199+58.32	0.00'	219285.756	375971.142
307	1199+58.22	36.27' RT	219286.182	376007.414
308	1197+08.03	35.61' RT	219036.011	376010.354
309	1196+78.03	35.53' RT	219006.013	376010.707
310	1195+46.97	30.82' LT	218874.011	375946.253
311	1195+58.97	30.79' LT	218886.011	375946.112
312	1195+58.02	35.21' RT	218886.011	376012.117
313	1195+46.02	35.18' RT	218874.011	376012.258

COURSE TABLE

COURSE	BEARING	DISTANCE
206 - 205	S89° 10' 29"W	31.14'
205 - 310	N00° 40' 24"W	120.01'
310 - 311	N00° 40' 24"W	12.00'
311 - 303	N00° 40' 24"W	120.01'
303 - 304	N00° 40' 24"W	30.00'
304 - 305	N00° 40' 24"W	249.33'
305 - 306	N89° 10' 29"E	29.73'
306 - 307	N89° 19' 36"E	36.27'
307 - 308	S00° 40' 24"E	250.19'
308 - 309	S00° 40' 24"E	30.00'
309 - 312	S00° 40' 24"E	120.01'
312 - 313	S00° 40' 24"E	12.00'
313 - 207	S00° 40' 24"E	119.06'
207 - 206	S89° 10' 29"W	34.86'

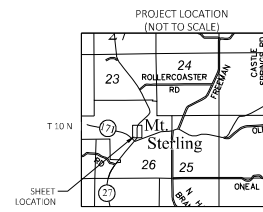
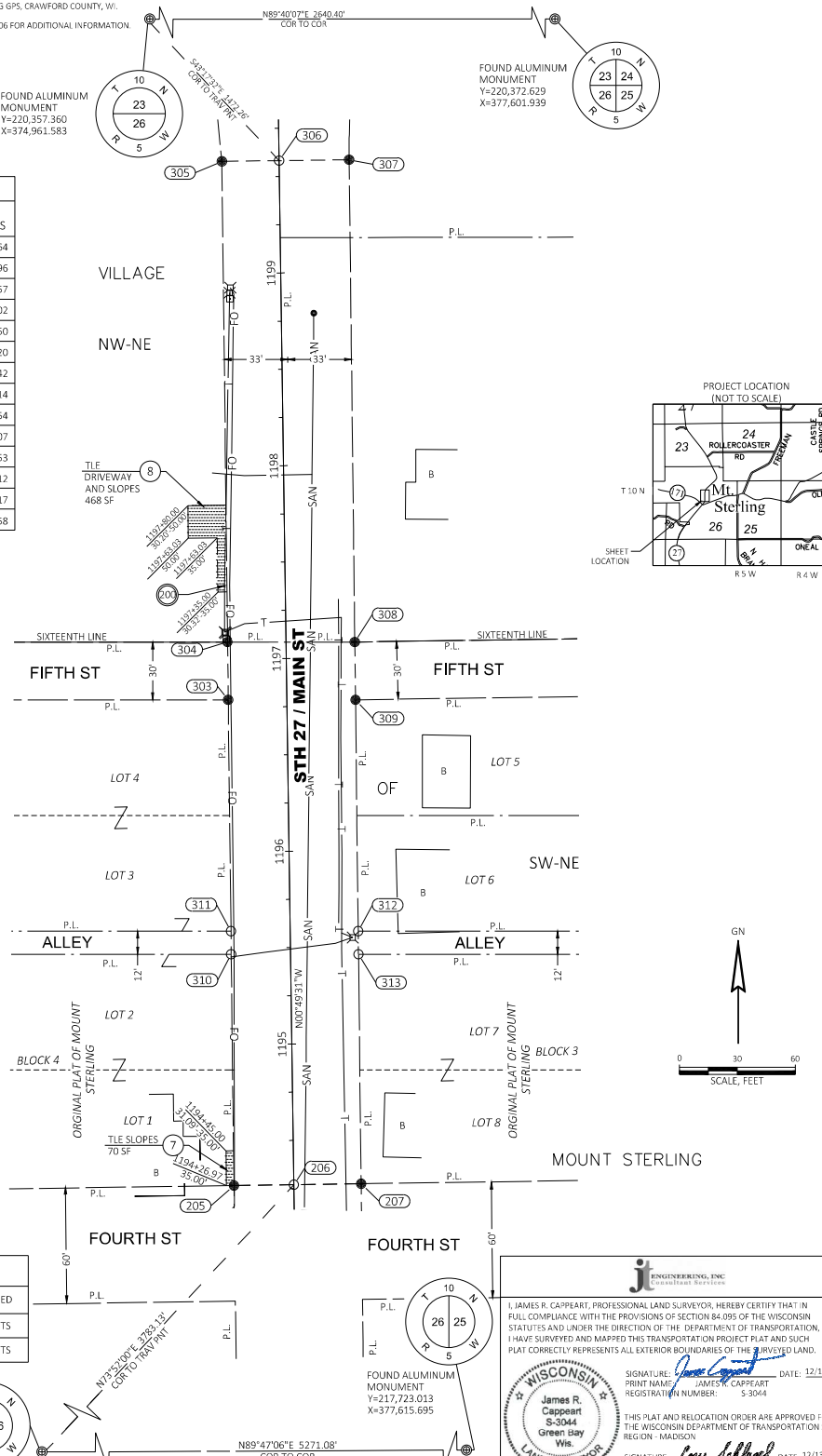
UTILITY INTERESTS REQUIRED

UTILITY NUMBER	OWNER (S)	INTEREST REQUIRED
200	CENTURYLINK	RELEASE OF RIGHTS
202	VALLEY RIDGE CLEAN WATER COMMISSION	RELEASE OF RIGHTS

200 VALLEY RIDGE CLEAN WATER COMMISSION
V. 548, P. 44, DOC: 242354 - PARCEL 7 BLANKET

202 CENTURYLINK
NO RECORDED EASEMENT - PARCEL 8

FOUND ALUMINUM MONUMENT
Y=217,703.224
X=372,344.656



J ENGINEERING, INC.
Professional Land Surveyors

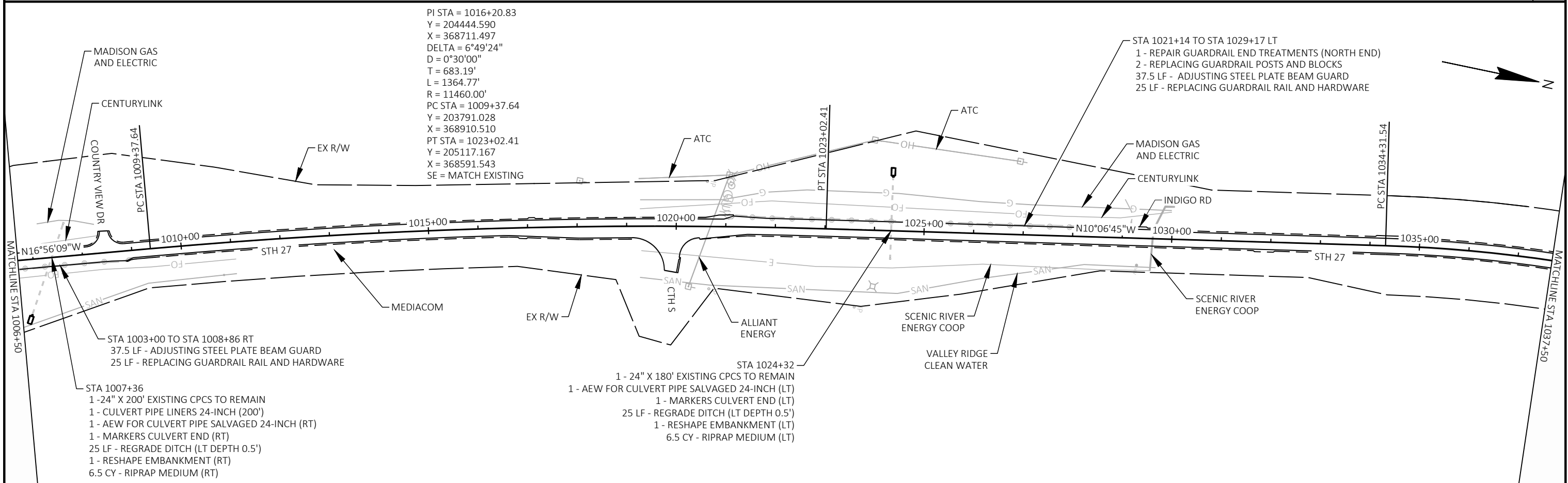
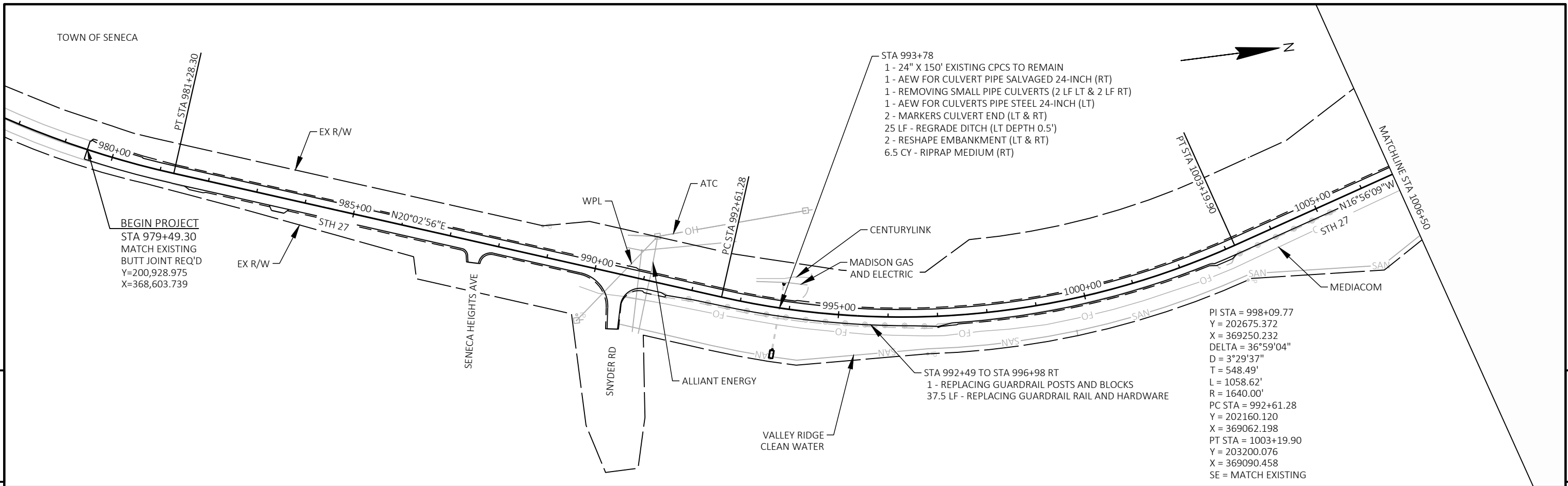
JAMES R. CAPPEART, PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF THE DEPARTMENT OF TRANSPORTATION, I HAVE SURVEYED AND MAPPED THIS TRANSPORTATION PROJECT PLAT AND SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.

SIGNATURE: *James R. Cappeart* DATE: 12/13/2021
PRINT NAME: JAMES R. CAPPEART
REGISTRATION NUMBER: S-3044

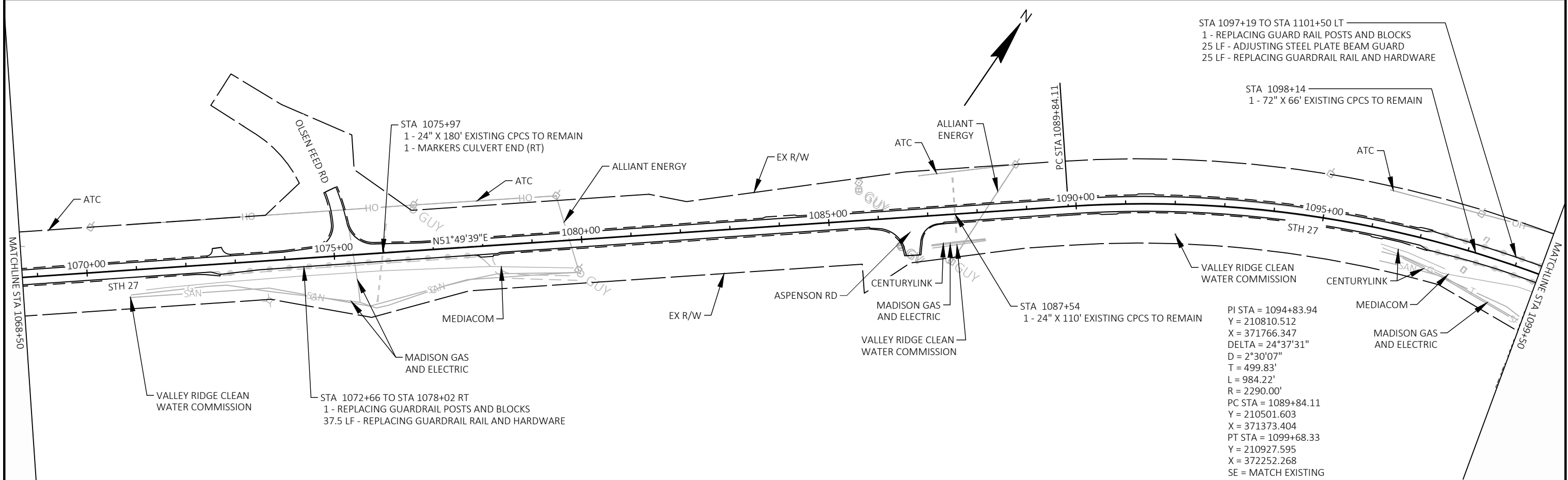
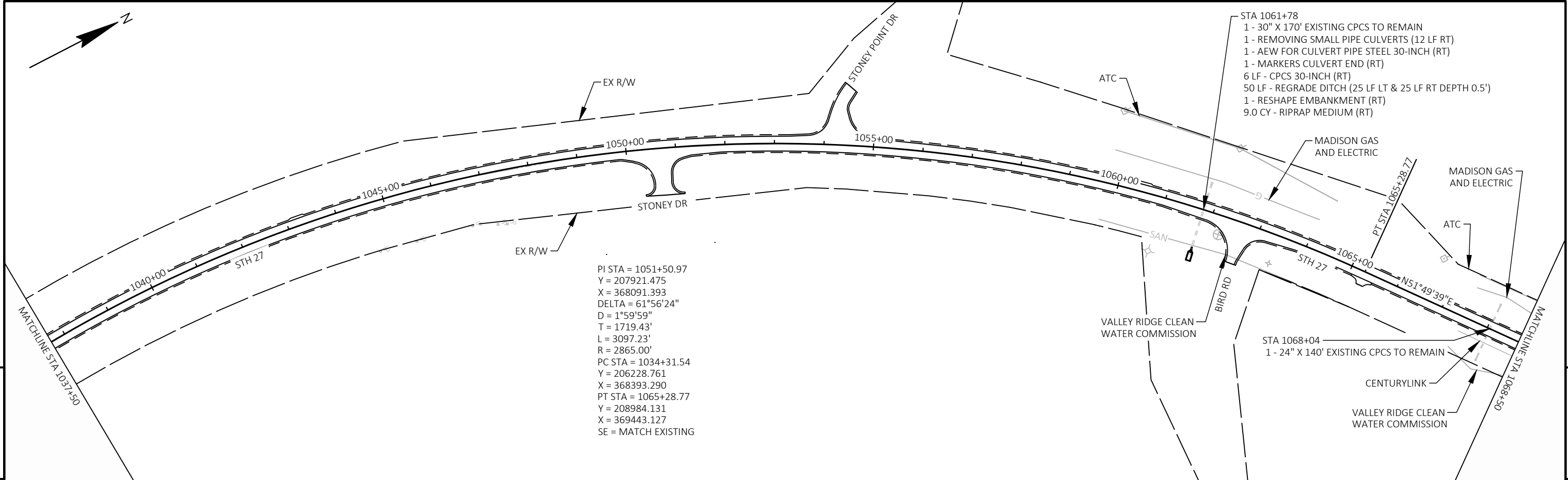
THIS PLAT AND RELOCATION ORDER ARE APPROVED FOR THE WISCONSIN DEPARTMENT OF TRANSPORTATION SW REGION - MADISON

SIGNATURE: *Cory Schugel* DATE: 12/13/2021
PRINT NAME: COR SCHUGEL

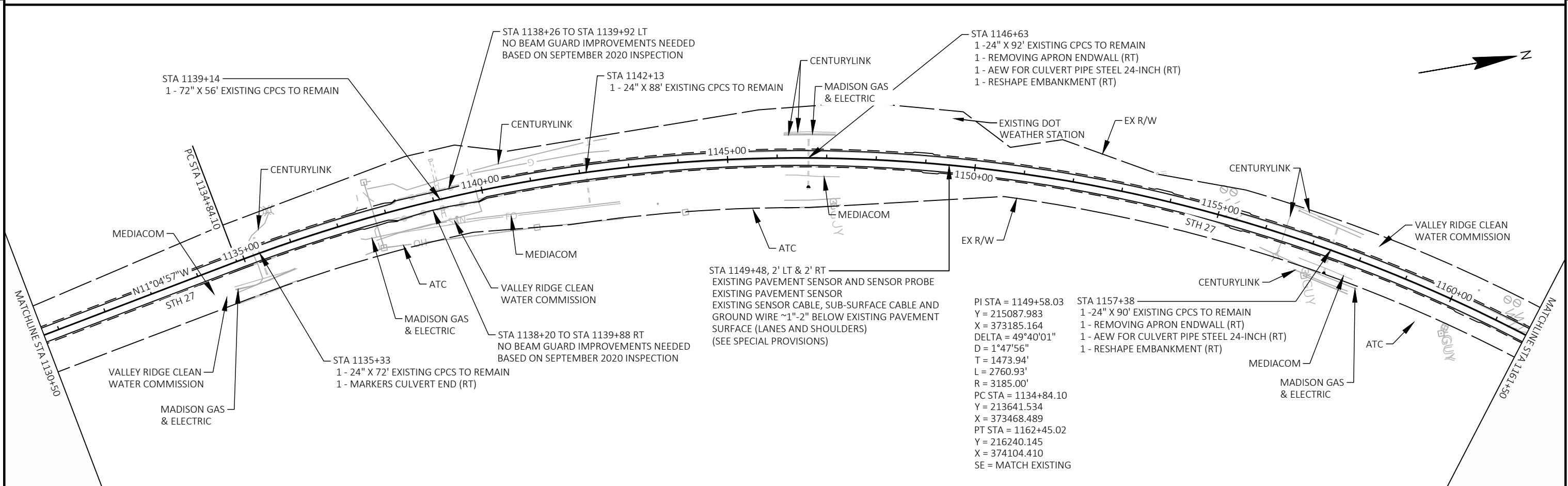
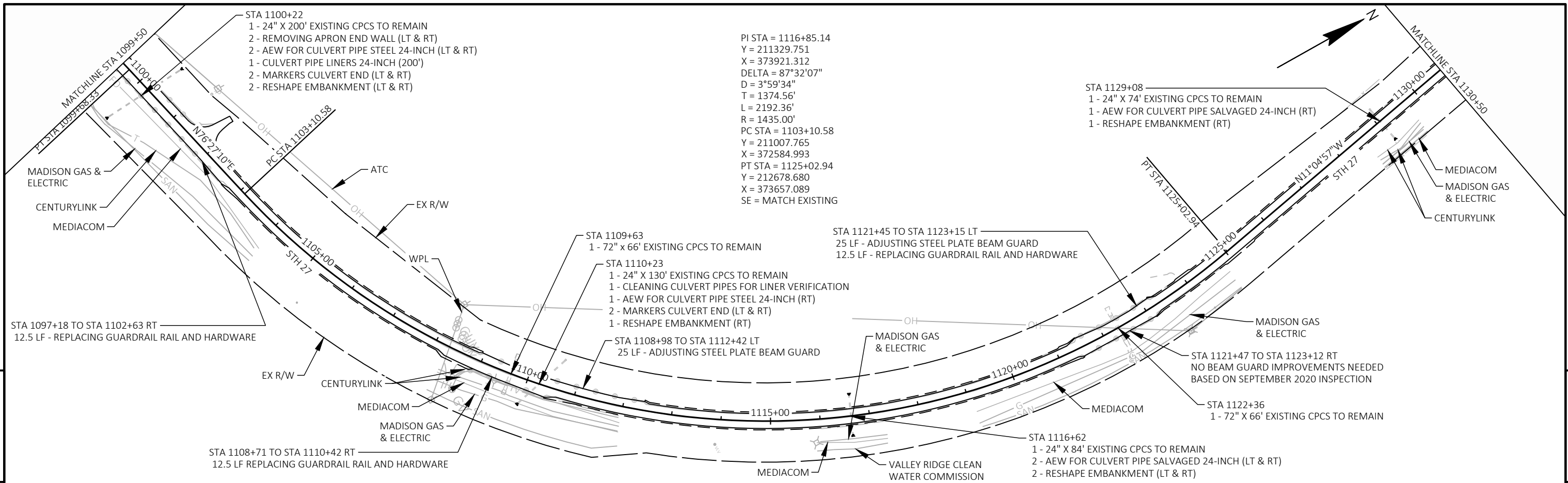
5540-02-21
1 IN 30 FT
PLOT SCALE: James Cappeart
PLOT BY: James Cappeart
PLOT DATE: 12/13/2021 3:32 PM
PLOT DATE: 12/13/2021 3:32 PM
FILE NAME: X:\PROJECTS\CRAWFORD\5540-02-01_STH_27_NORTH AVE TO CTH C\DESIGN\CDRAW\DWG\0401.DWG
APPRAISAL PLAT DATE: DECEMBER 13, 2021



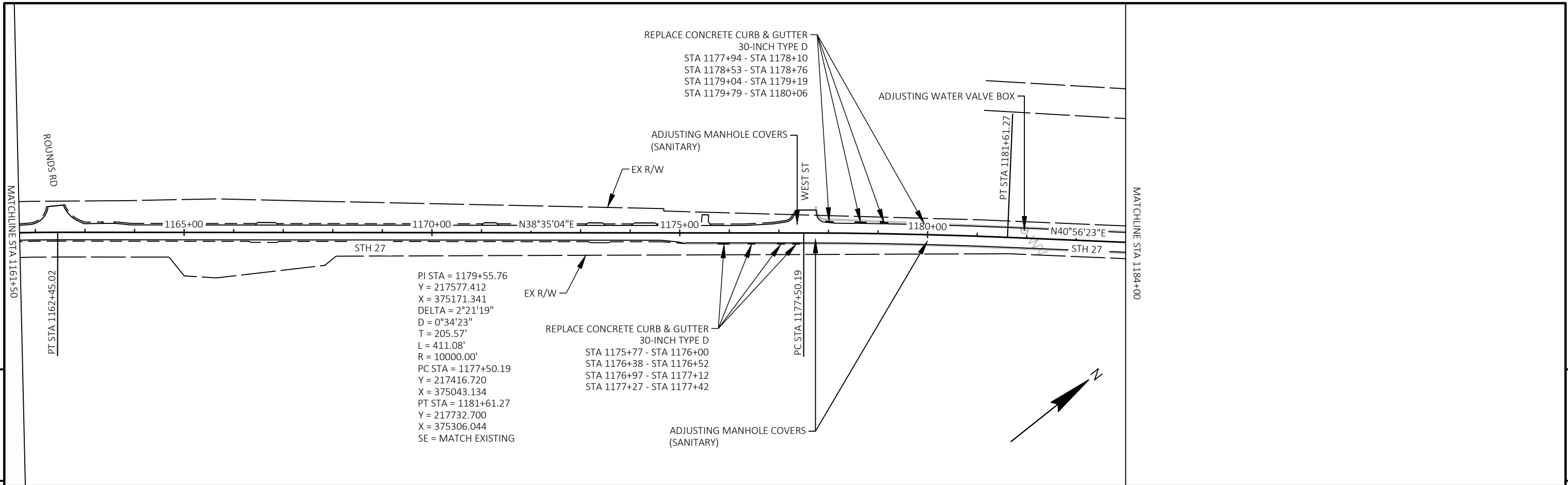
PROJECT NO: 5540-02-71	HWY: STH 27	COUNTY: CRAWFORD	PLAN	SHEET	E
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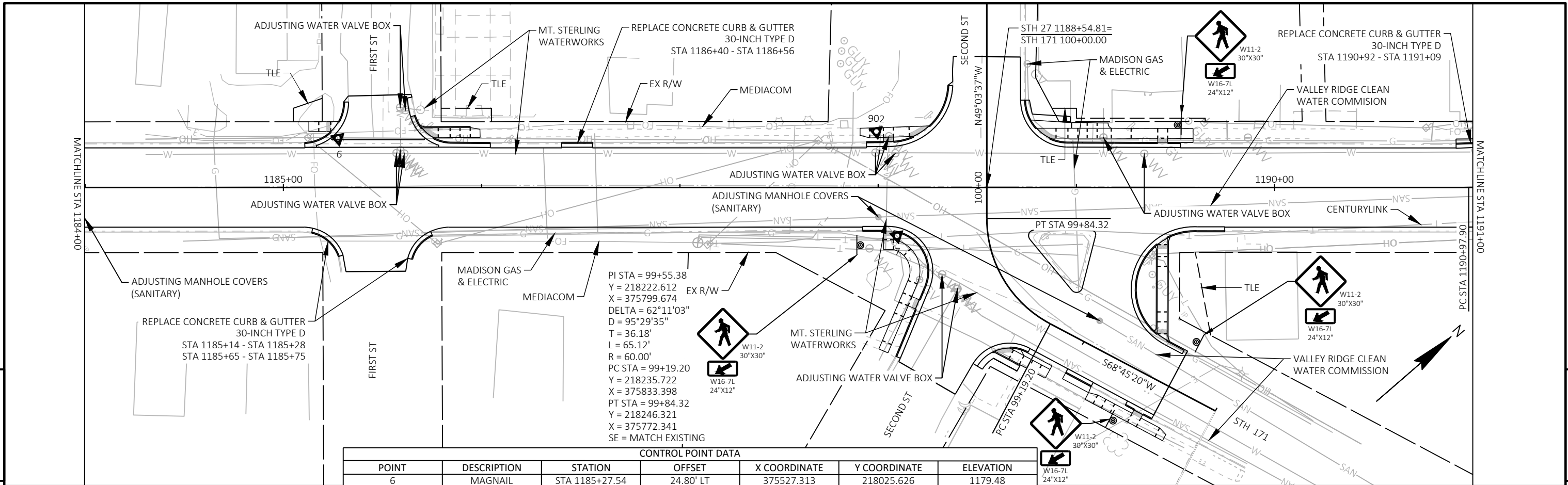


PROJECT NO: 5540-02-71	HWY: STH 27	COUNTY: CRAWFORD	PLAN	SHEET	E
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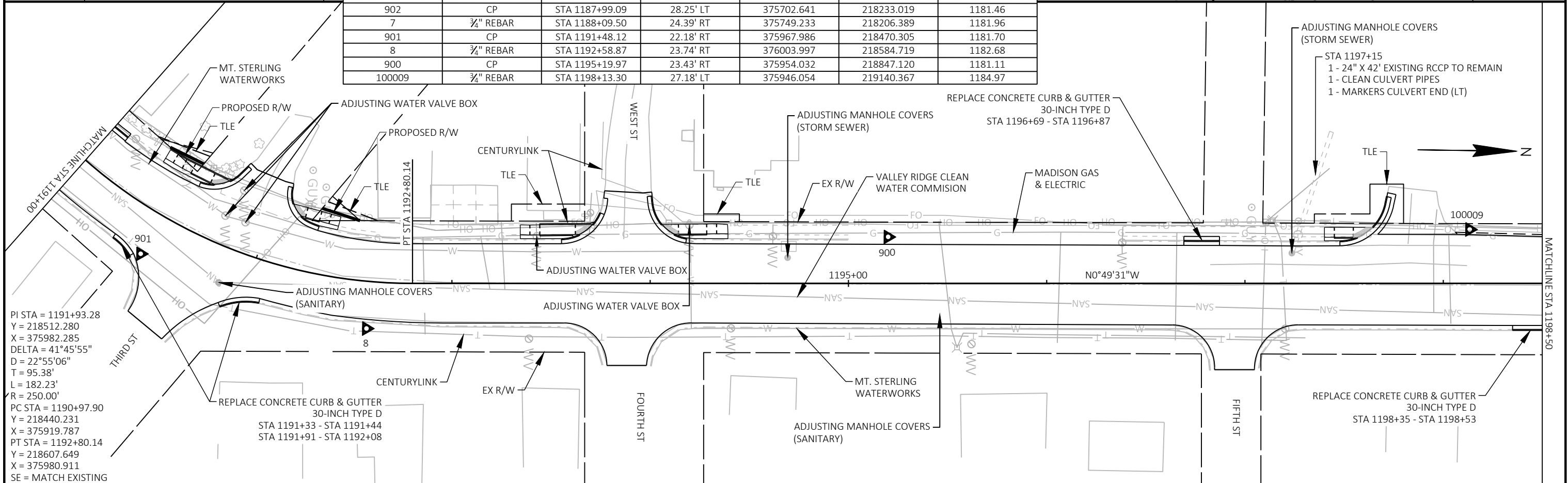


PROJECT NO: 5540-02-71	HWY: STH 27	COUNTY: CRAWFORD	PLAN	SHEET	E
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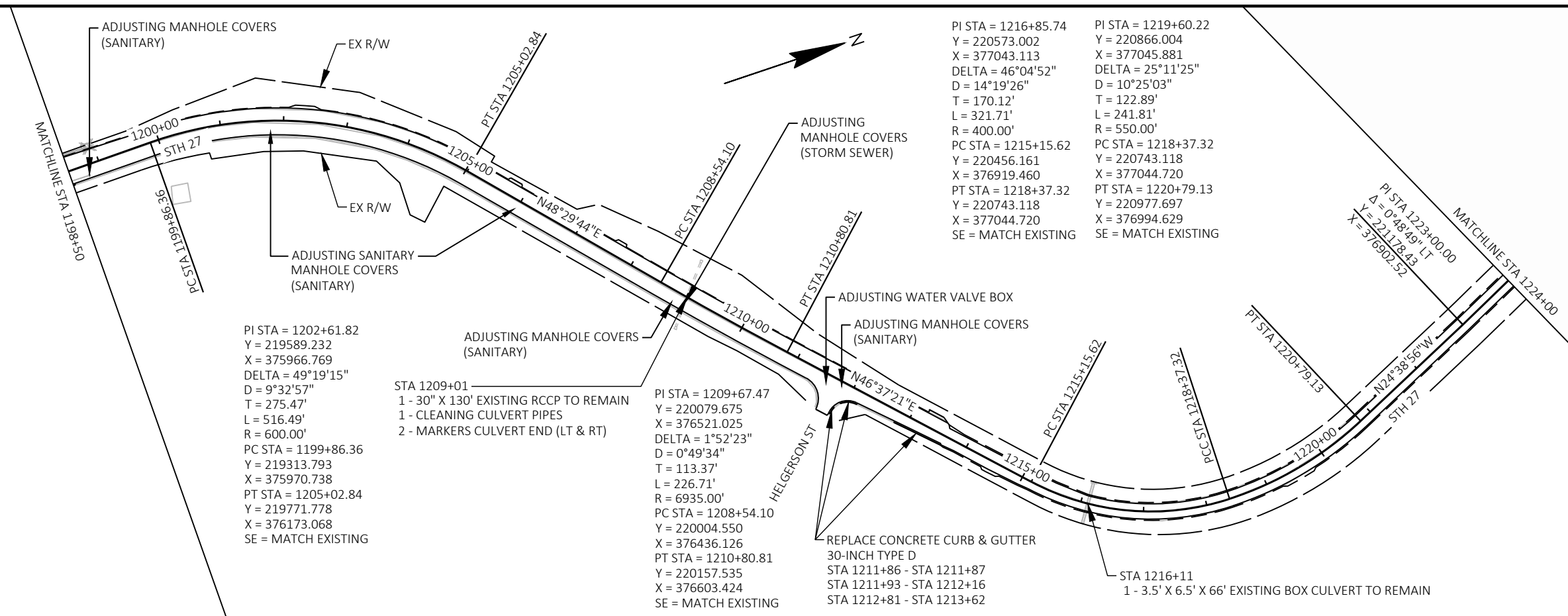




CONTROL POINT DATA						
POINT	DESCRIPTION	STATION	OFFSET	X COORDINATE	Y COORDINATE	ELEVATION
6	MAGNAIL	STA 1185+27.54	24.80' LT	375527.313	218025.626	1179.48
902	CP	STA 1187+99.09	28.25' LT	375702.641	218233.019	1181.46
7	3/4" REBAR	STA 1188+09.50	24.39' RT	375749.233	218206.389	1181.96
901	CP	STA 1191+48.12	22.18' RT	375967.986	218470.305	1181.70
8	3/4" REBAR	STA 1192+58.87	23.74' RT	376003.997	218584.719	1182.68
900	CP	STA 1195+19.97	23.43' RT	375954.032	218847.120	1181.11
100009	3/4" REBAR	STA 1198+13.30	27.18' LT	375946.054	219140.367	1184.97



PROJECT NO: 5540-02-71 HWY: STH 27 COUNTY: CRAWFORD PLAN SHEET E



PI STA = 1216+85.74 Y = 220573.002 X = 377043.113 DELTA = 46°04'52" D = 14°19'26" T = 170.12' L = 321.71' R = 400.00'	PI STA = 1219+60.22 Y = 220866.004 X = 377045.881 DELTA = 25°11'25" D = 10°25'03" T = 122.89' L = 241.81' R = 550.00'
PC STA = 1215+15.62 Y = 220456.161 X = 376919.460 PT STA = 1218+37.32 Y = 220743.118 X = 377044.720 SE = MATCH EXISTING	PC STA = 1218+37.32 Y = 220743.118 X = 377044.720 PT STA = 1220+79.13 Y = 220977.697 X = 376994.629 SE = MATCH EXISTING

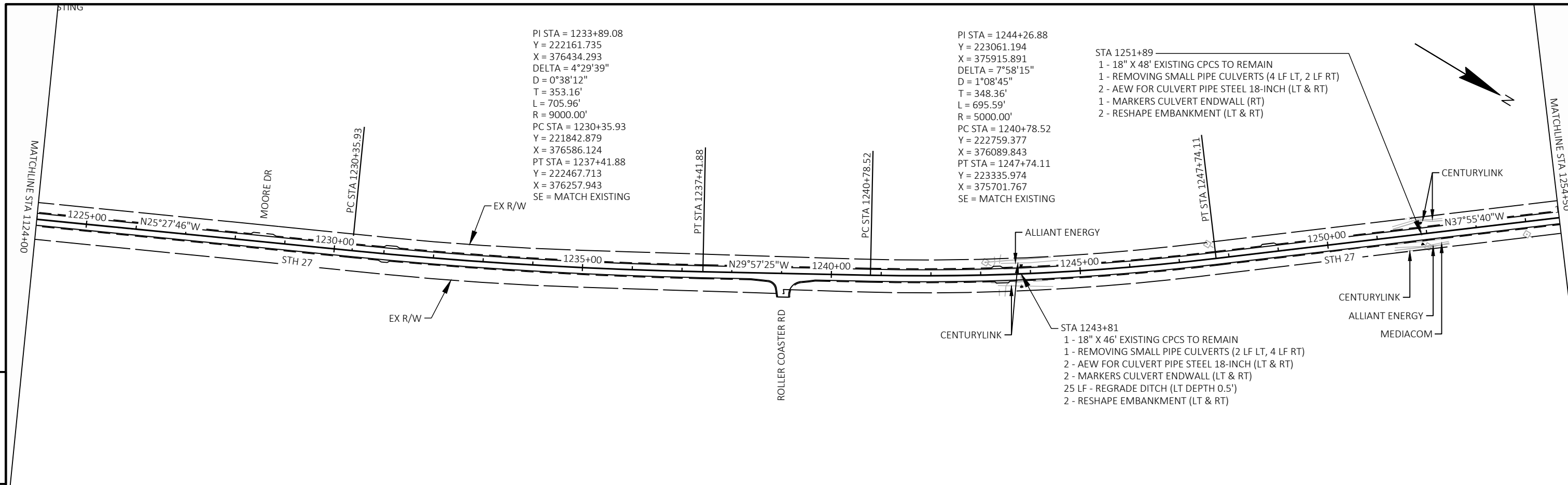
PI STA = 1202+61.82
Y = 219589.232
X = 375966.769
DELTA = 49°19'15"
D = 9°32'57"
T = 275.47'
L = 516.49'
R = 600.00'
PC STA = 1199+86.36
Y = 219313.793
X = 375970.738
PT STA = 1205+02.84
Y = 219771.778
X = 376173.068
SE = MATCH EXISTING

STA 1209+01
1 - 30" X 130' EXISTING RCCP TO REMAIN
1 - CLEANING CULVERT PIPES
2 - MARKERS CULVERT END (LT & RT)

PI STA = 1209+67.47
Y = 220079.675
X = 376521.025
DELTA = 1°52'23"
D = 0°49'34"
T = 113.37'
L = 226.71'
R = 6935.00'
PC STA = 1208+54.10
Y = 220004.550
X = 376436.126
PT STA = 1210+80.81
Y = 220157.535
X = 376603.424
SE = MATCH EXISTING

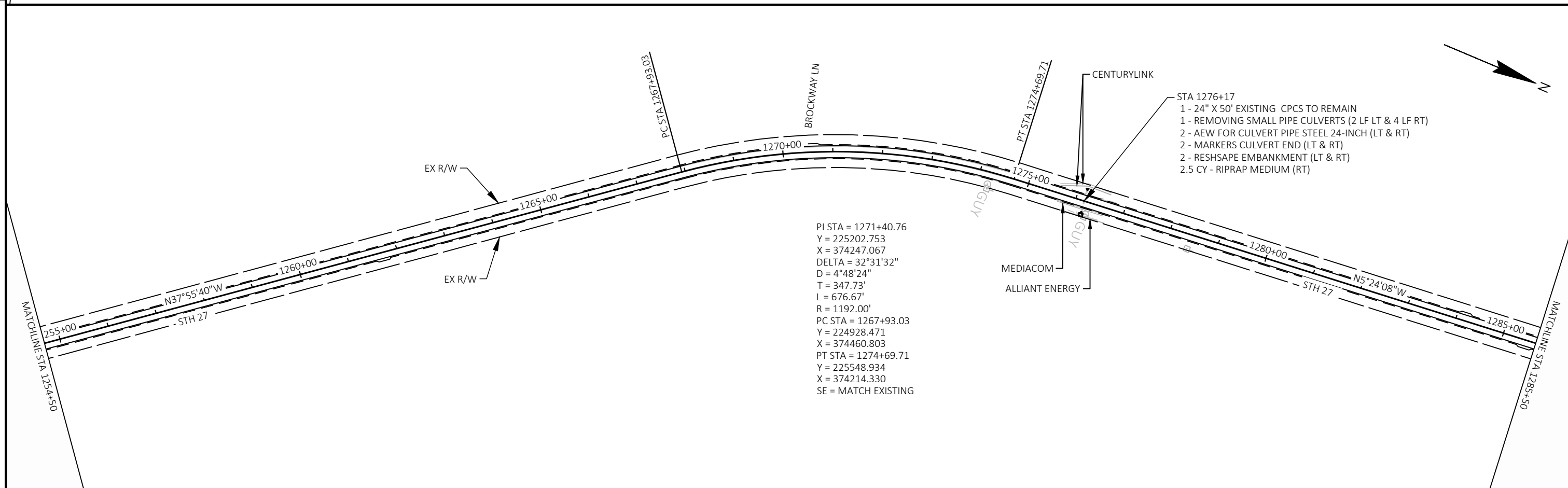
REPLACE CONCRETE CURB & GUTTER
30-INCH TYPE D
STA 1211+86 - STA 1211+87
STA 1211+93 - STA 1212+16
STA 1212+81 - STA 1213+62

STA 1216+11
1 - 3.5' X 6.5' X 66' EXISTING BOX CULVERT TO REMAIN

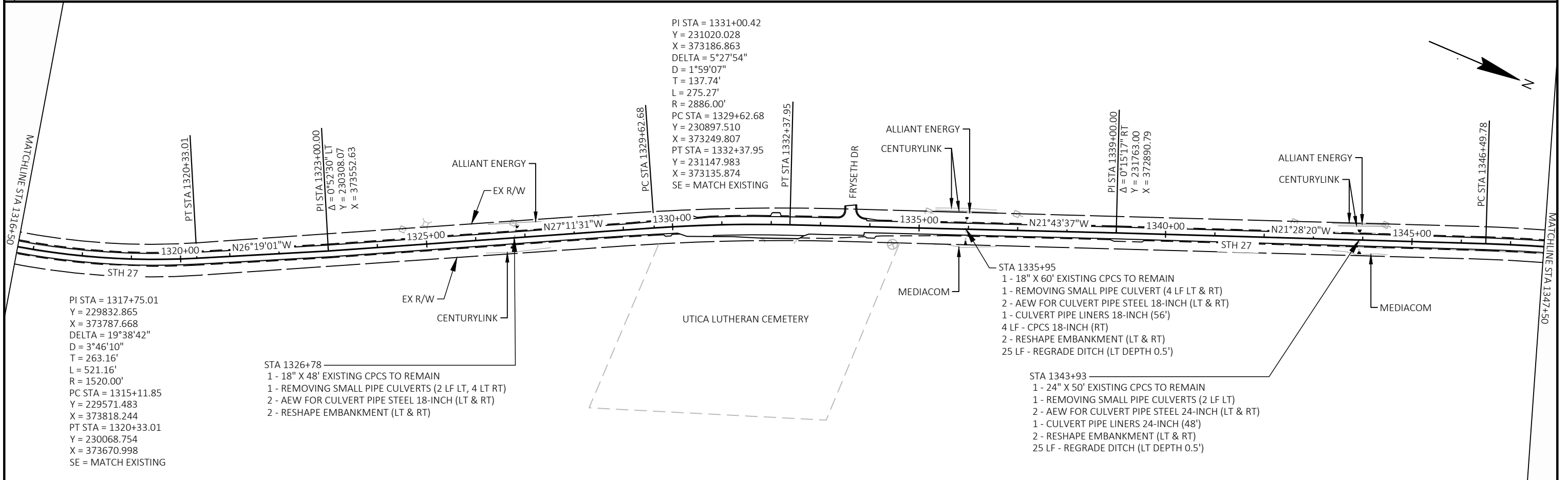
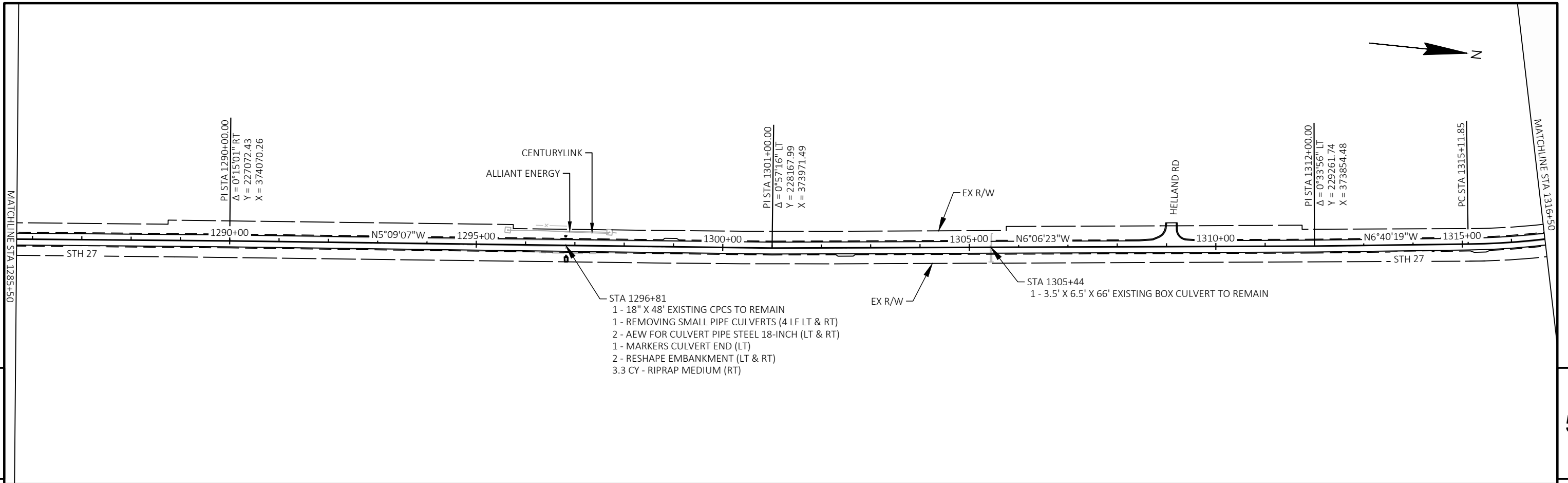


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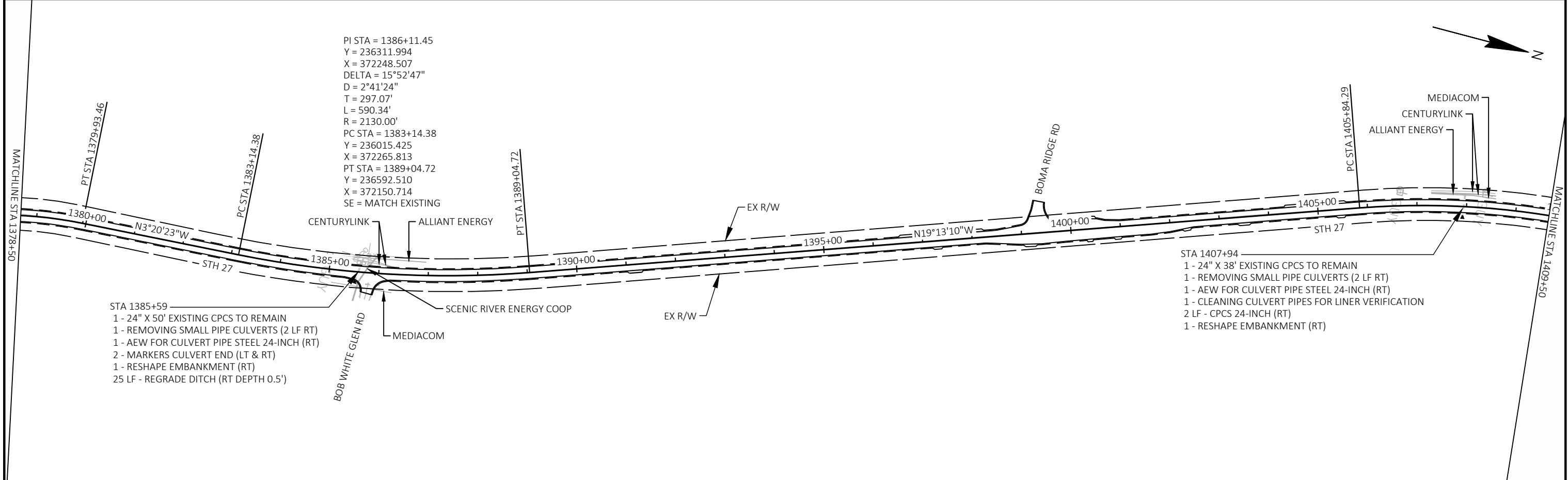
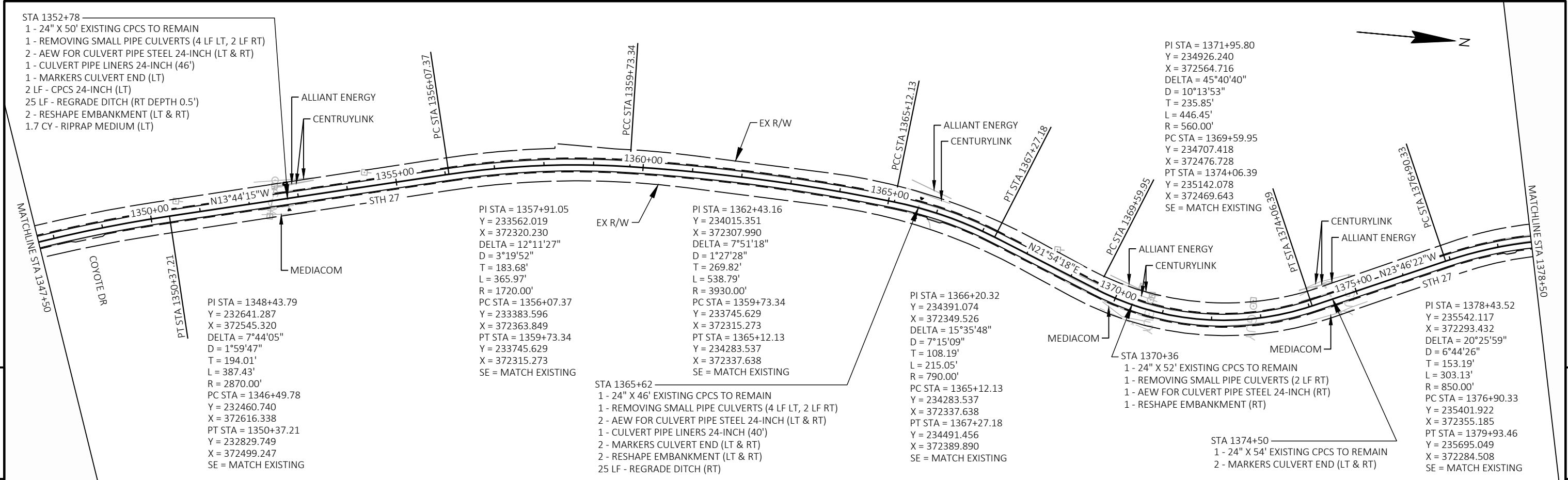
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PROJECT NO: 5540-02-71	HWY: STH 27	COUNTY: CRAWFORD	PLAN	SHEET	E
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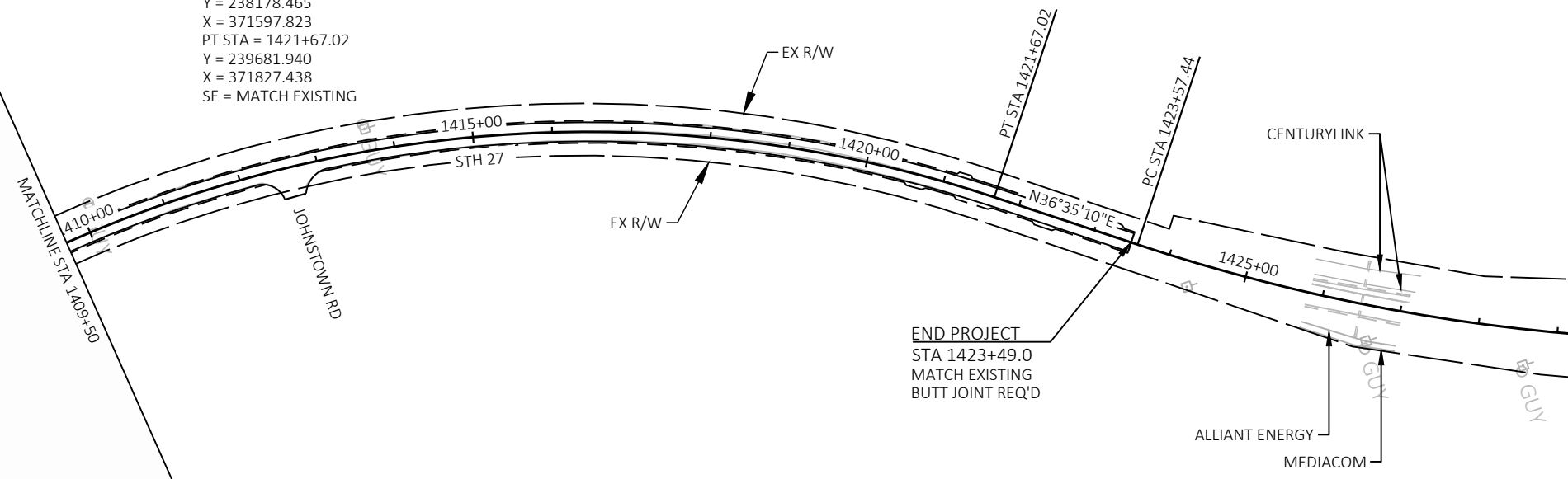


PROJECT NO: 5540-02-71	HWY: STH 27	COUNTY: CRAWFORD	PLAN	SHEET	E
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PROJECT NO: 5540-02-71	HWY: STH 27	COUNTY: CRAWFORD	PLAN	SHEET	E
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PI STA = 1414+44.78
 Y = 238990.997
 X = 371314.561
 DELTA = 55°48'19"
 D = 3°31'33"
 T = 860.49'
 L = 1582.73'
 R = 1625.00'
 PC STA = 1405+84.29
 Y = 238178.465
 X = 371597.823
 PT STA = 1421+67.02
 Y = 239681.940
 X = 371827.438
 SE = MATCH EXISTING



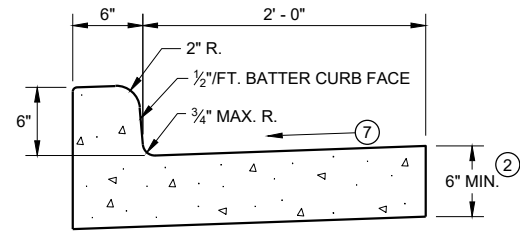
END PROJECT
 STA 1423+49.0
 MATCH EXISTING
 BUTT JOINT REQ'D

5

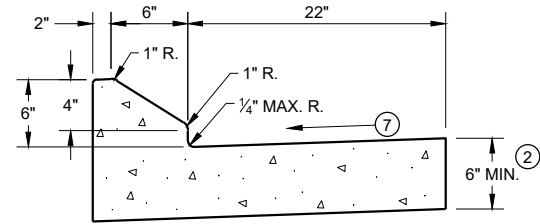
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Standard Detail Drawing List

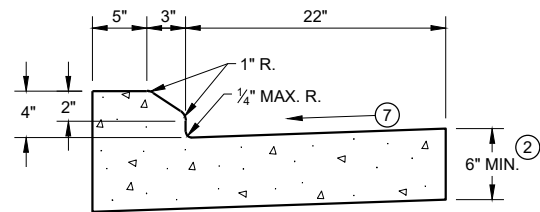
08D01-23A	CONCRETE CURB & GUTTER
08D01-23B	CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS
08D05-20A	CURB RAMPS TYPES 1 AND 1-A
08D05-20B	CURB RAMPS TYPES 2 AND 3
08D05-20C	CURB RAMPS TYPES 4A AND 4A1
08D05-20D	CURB RAMPS TYPE 4B AND 4B1
08D05-20E	CURB RAMPS TYPES 5, 6, 7A, 7B & 8
08D05-20F	CURB RAMPS RADIAL DETECTABLE WARNING FIELD APPLICATIONS
08D05-20G	CURB RAMPS RECTANGULAR AND RADIAL DETECTABLE WARNING PLATES
08D16-11	CONCRETE GUTTER, CURB AND GUTTER AND PAVEMENT TIES
08D22-01	DRIVEWAYS WITHOUT CURB & GUTTER RESURFACING PROJECTS RURAL
08E09-06	SILT FENCE
08E10-02	INLET PROTECTION TYPE A, B, C AND D
08E15-01	CULVERT PIPE CHECK
08F01-11	APRON ENDWALLS FOR CULVERT PIPE
13A11-04A	CENTERLINE RUMBLE STRIPS - ASPHALT
13A11-04B	CENTERLINE RUMBLE STRIPS - CONCRETE
13A11-04D	CENTERLINE RUMBLE STRIPS - INTERSECTIONS, DRIVEWAYS, BRIDGES, RAILROADS
13C19-03	HMA LONGITUDINAL JOINTS
14B15-11A	STEEL PLATE BEAM GUARD, CLASS "A" INSTALLATION & ELEMENTS
14B15-11B	STEEL PLATE BEAM GUARD, CLASS "A" INSTALLATION & ELEMENTS
14B15-11C	STEEL PLATE BEAM GUARD, CLASS "A", INSTALLATION & ELEMENTS
14B29-01	SAFETY EDGE
15A03-02A	FLEXIBLE MARKER POST FOR CULVERT END
15A03-02B	FLEXIBLE MARKER POST FOR CULVERT END
15C03-05	BARRICADES AND SIGNS FOR SIDEROAD CLOSURES
15C04-05	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M. P. H. OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC
15C07-15C	PAVEMENT MARKING ARROWS
15C08-23A	PERMANENT LONGITUDINAL PAVEMENT MARKINGS
15C08-23B	TEMPORARY LONGITUDINAL PAVEMENT MARKING
15C08-23C	PAVEMENT MARKING (TURN LANES)
15C08-23D	PAVEMENT MARKING (TURN LANES)
15C11-10B	CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS
15C12-09A	TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION
15C12-09B	TRAFFIC CONTROL, LANE CLOSURE WITH AUTOMATED FLAGGER ASSISTANCE DEVICE
15C19-08A	MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY
15C33-04	STOP LINE AND CROSSWALK PAVEMENT MARKING
15C35-06A	PAVEMENT MARKING (INTERSECTIONS)
15D28-04	TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY
15D30-09A	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09B	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09C	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09D	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09E	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09F	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09G	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09H	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09I	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09J	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09K	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09L	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D39-02	TRAFFIC CONTROL, DROP-OFF SIGNING
15D44-02	TRAFFIC CONTROL, SIGNING ON ROADWAYS WITH MILLED SURFACES
15D45-03	TRAFFIC CONTROL, SIGNING ON ROADWAYS WITH LOOSE GRAVEL



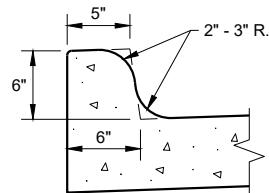
TYPES A¹ & D



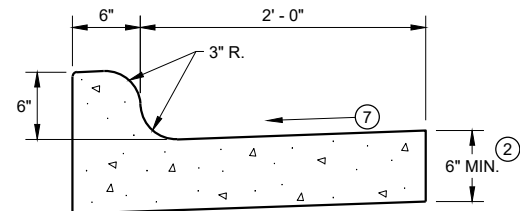
6" SLOPED CURB TYPES G¹ & J



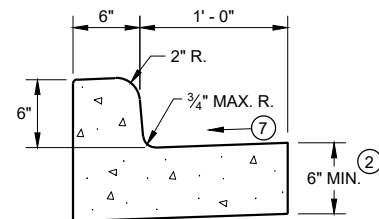
4" SLOPED CURB TYPES G¹ & J



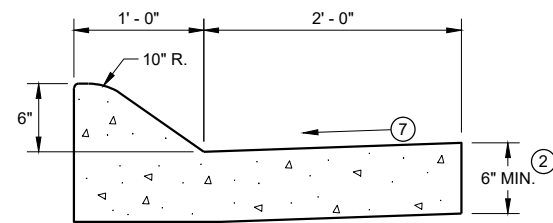
TYPES K¹ & L
(OPTIONAL CURB SHAPE)



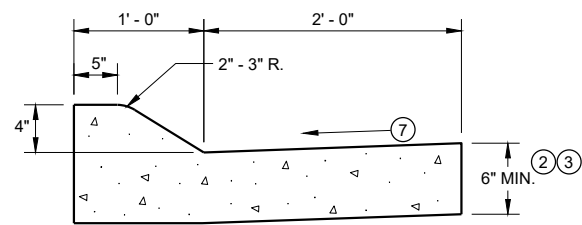
TYPES K¹ & L
CONCRETE CURB AND GUTTER 30"



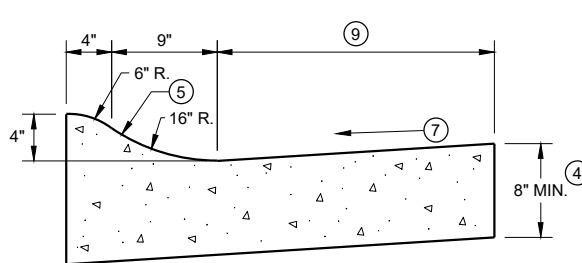
TYPES A¹ & D
CONCRETE CURB AND GUTTER 18"



6" SLOPED CURB TYPES A¹ & D

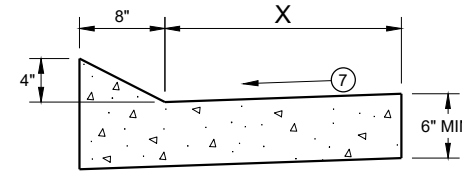


4" SLOPED CURB TYPES A¹ & D
CONCRETE CURB AND GUTTER 36"



4" SLOPED CURB TYPES R¹ & T

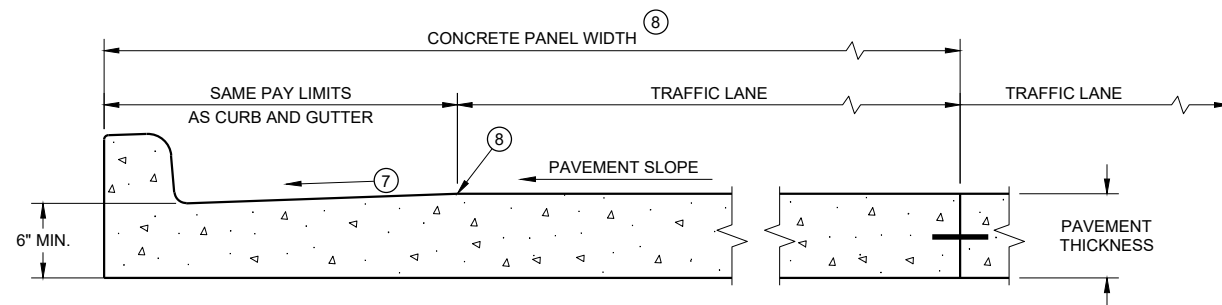
TBT & TBTT	X
30"	22"
36"	28"



TYPES TBT & TBTT¹
CONCRETE CURB AND GUTTER

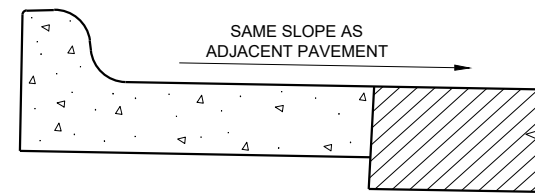
PAVEMENT THICKNESS AND MAXIMUM CONCRETE PANEL WIDTH TABLE

PAVEMENT THICKNESS	MAXIMUM PANEL WIDTH
LESS THAN 10"	12'
10" & ABOVE	15'



PARTIAL SECTION OF PAVEMENT* WITH INTEGRAL CURB AND GUTTER

* BIKE LANE IS NOT SHOWN



REVERSE SLOPE GUTTER⁶
(TYPICAL FOR ALL CURB & GUTTER TYPES)

GENERAL NOTES

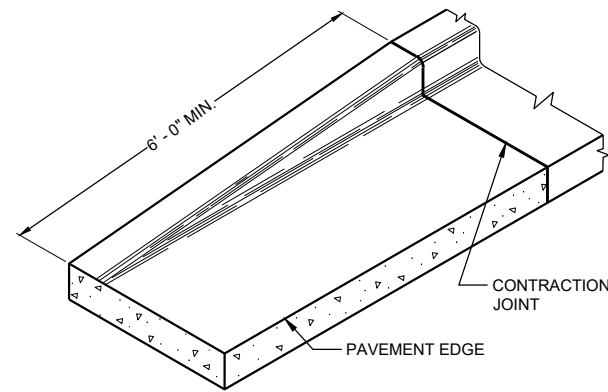
DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

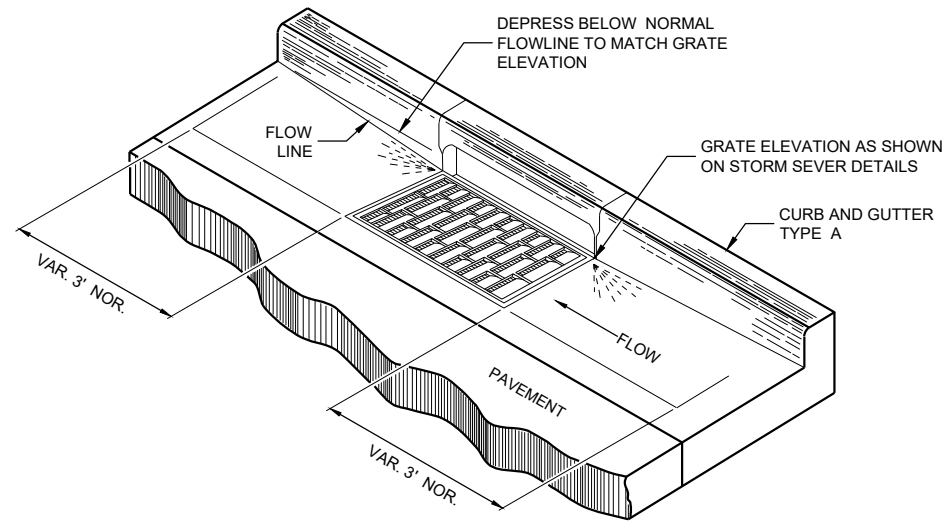
INTEGRAL CURB AND GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB AND GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ③ USE 8" MINIMUM GUTTER THICKNESS WHEN USED WITH AN ADJACENT CONCRETE TRUCK APRON PLACED BEHIND BACK OF CURB.
- ④ THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑤ UNLESS OTHERWISE NOTED, FOR STAKING PURPOSES THE FACE OF CURB IS 6" FROM THE BACK OF CURB.
- ⑥ WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.
- ⑦ USE 4% GUTTER CROSS SLOPE UNLESS OTHERWISE NOTED IN THE PLANS.
- ⑧ INCLUDE LONGITUDINAL JOINT AND TIE BARS ALONG LANE EDGE WHEN CONCRETE PANEL WIDTH EXCEEDS THE MAXIMUM WIDTH PER TABLE BELOW. LONGITUDINAL JOINT(S) ARE NOT ALLOWED WITHIN TRAFFIC LANES AND BIKE LANES. LONGITUDINAL JOINT MAY BE SAWED.
- ⑨ CONCRETE CURB AND GUTTER 4-INCH SLOPED 30-INCH TYPE "R" AND "T" = 17 INCHES
CONCRETE CURB AND GUTTER 4-INCH SLOPED 36-INCH TYPE "R" AND "T" = 23 INCHES



END SECTION CURB AND GUTTER



DETAIL OF CURB AND GUTTER AT INLETS

(TYPICAL H INLET COVER SHOWN)

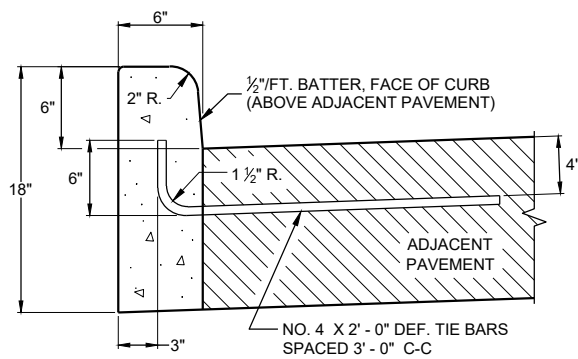
GENERAL NOTES

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

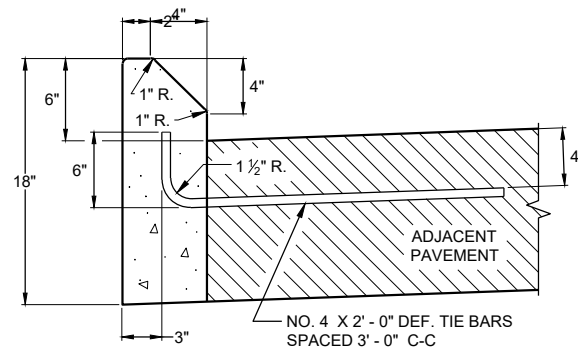
PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

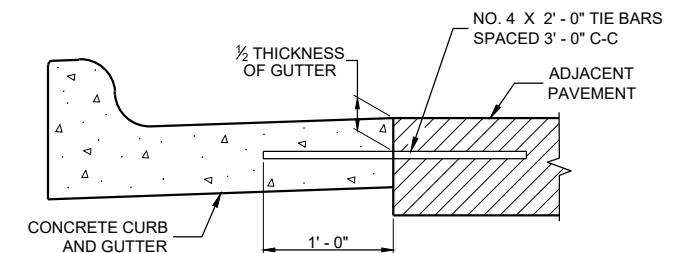
- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑩ REFER TO SDD 08D18 AND 08D19 FOR ADDITIONAL DRIVEWAY ENTRANCE CURB DETAILS.
- ⑪ PLACE 1" THICK EXPANSION JOINT MATERIAL BETWEEN VERTICAL FACE CURB TYPES EXTENDING FROM THE TOP OF CURB TO 1 INCH BELOW THE ADJOINING CONCRETE SURFACE. RIGID CONCRETE STRUCTURES INCLUDE RAISED CONCRETE MEDIANS, CONCRETE SAFETY ISLANDS, SPLITTER ISLANDS, OR LOCATIONS IDENTIFIED ON THE PLANS.



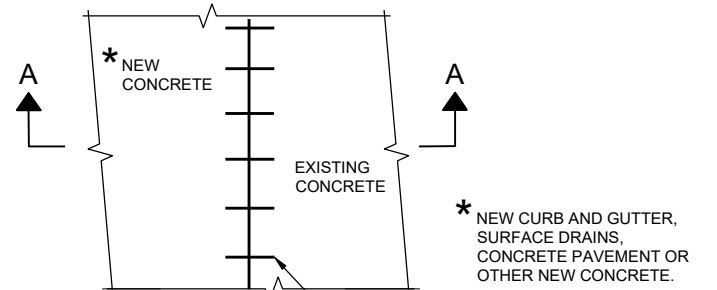
TYPES A^① & D



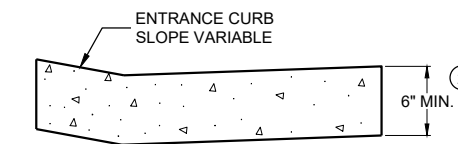
**TYPES G^① & J
CONCRETE CURB**



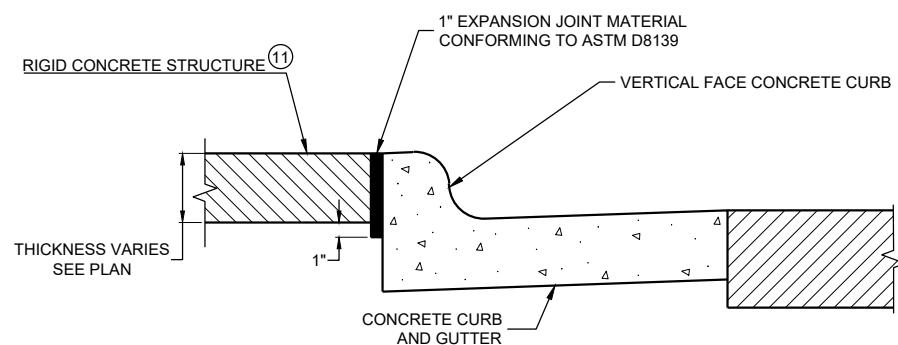
TYPICAL TIE BAR LOCATION^①



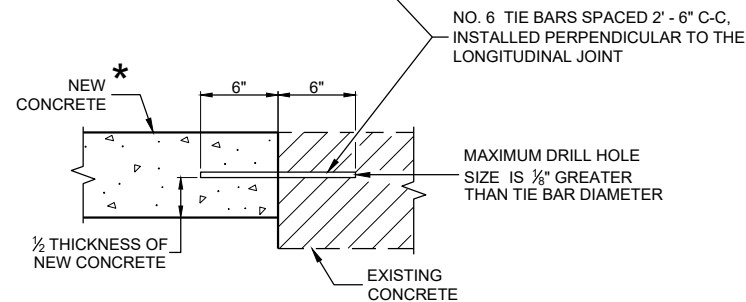
PLAN VIEW



**DRIVEWAY ENTRANCE CURB^⑩
(WHEN DIRECTED BY THE ENGINEER)**



EXPANSION JOINT DETAIL FOR VERTICAL CURB ABUTTING A RIGID STRUCTURE^⑪



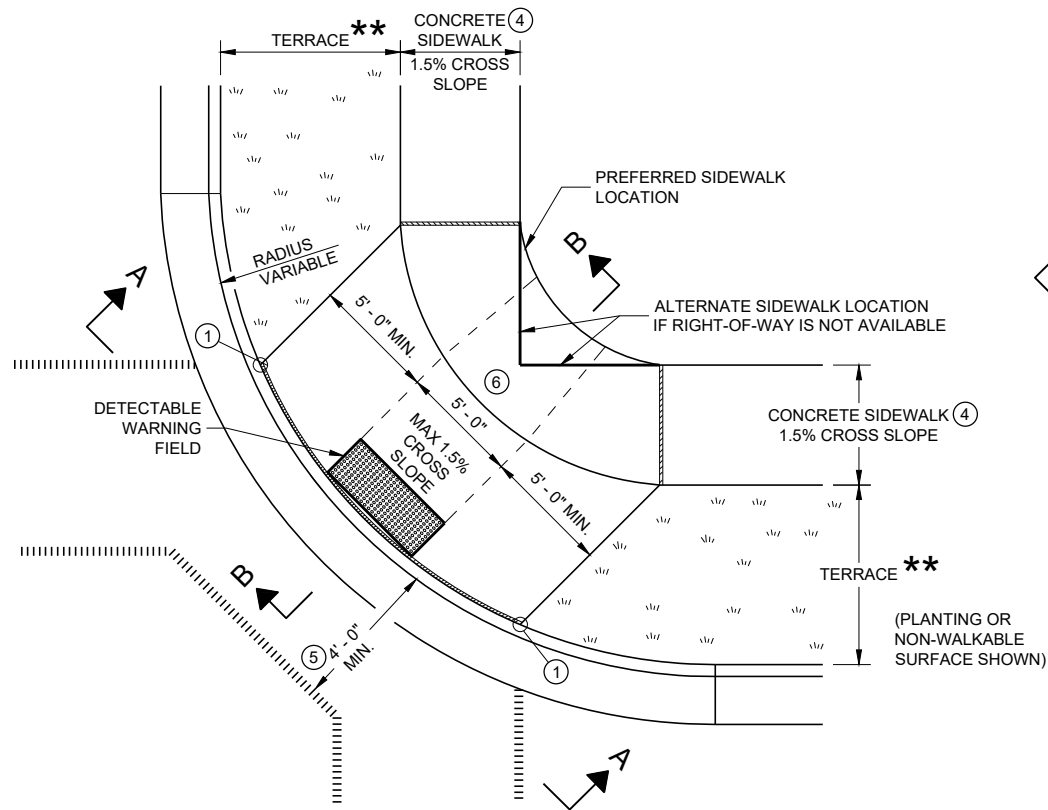
**SECTION A - A
TIE BARS DRILLED INTO EXISTING PAVEMENT**

CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS

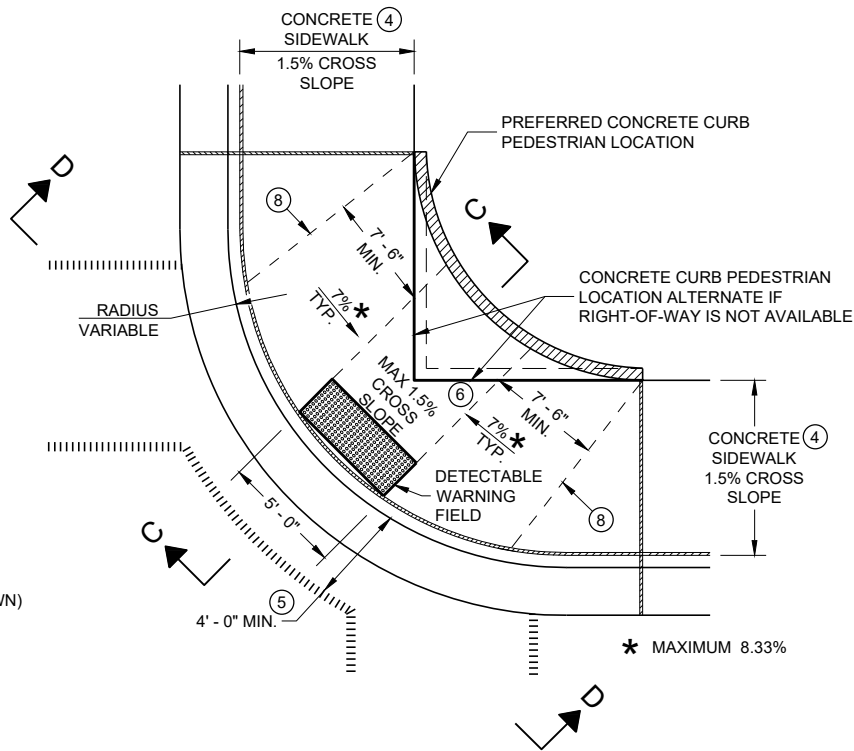
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE May 2023 /S/ Rodney Taylor
ROADWAY STANDARDS DEVELOPMENT ENGINEER

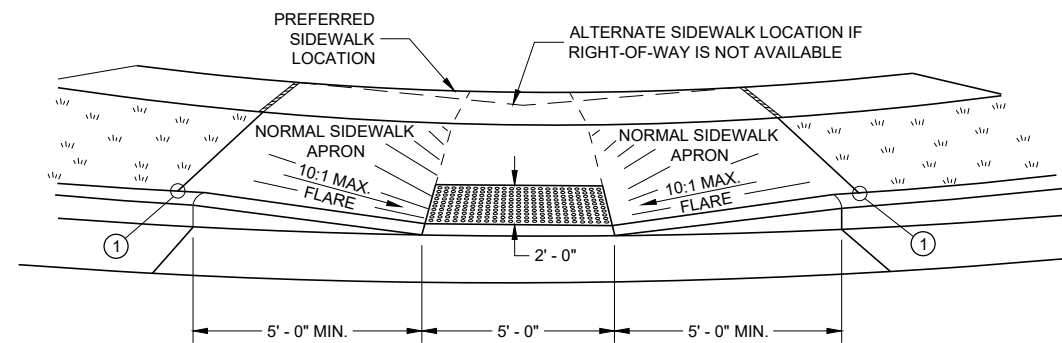
FHWA



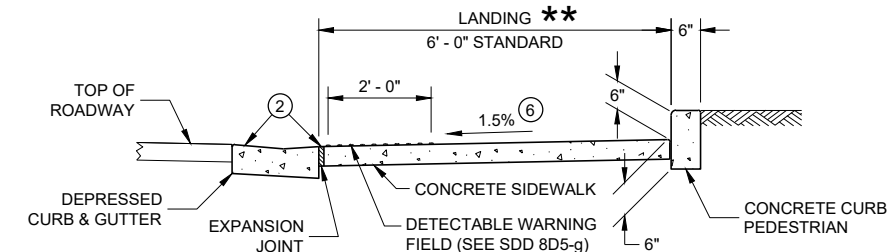
**PLAN VIEW
CURB RAMP TYPE 1
(CENTER OF CORNER RADIUS)**



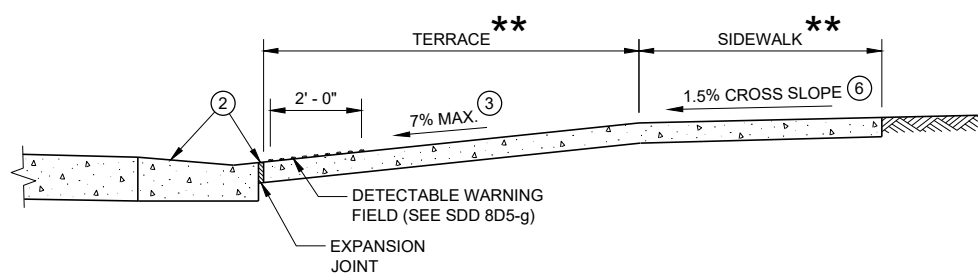
**PLAN VIEW
CURB RAMP TYPE 1 - A
(NO TERRACE)**



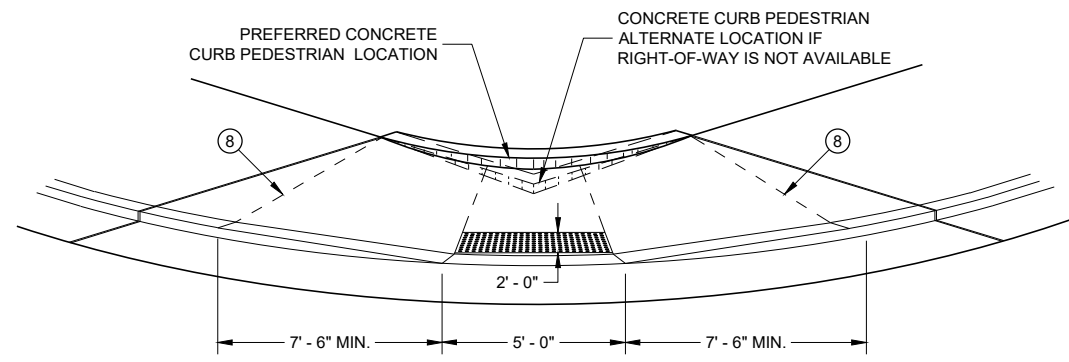
VIEW A - A FOR TYPE 1



SECTION C - C FOR TYPE 1 - A



SECTION B - B FOR TYPE 1



VIEW D - D FOR TYPE 1 - A

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
 DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

WHEN NECESSARY, THE SIDEWALK ELEVATION MAY BE LOWERED TO MEET THE HIGH POINT ON THE RAMP.
 TYPE 1 CURB RAMPS SHALL HAVE A NORMAL SIDEWALK APRON AND CURB ON BOTH SIDES OF RAMP.

DETECTABLE WARNING FIELD SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS "CURB RAMP DETECTABLE WARNING FIELD". THE CONCRETE PEDESTRIAN CURB, IF NEEDED, SHALL BE MEASURED AND PAID BY THE LINEAR FOOT AS "CONCRETE CURB PEDESTRIAN". CONCRETE SIDEWALK IN THE CURB RAMP AREA SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS CONCRETE SIDEWALK, INCLUDING THE AREA UNDER THE DETECTABLE WARNING FIELD.

SELECT CURB RAMP DETECTABLE WARNING FIELD MATERIALS AND DEVICES FROM THE DEPARTMENT'S APPROVED MATERIALS LIST. THE COLOR OF THE DETECTABLE WARNING FIELD IS SPECIFIED ELSEWHERE AND IS INCIDENTAL TO THE BID ITEM OF "CURB RAMP DETECTABLE WARNING FIELD"

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

SURFACE TEXTURE OF THE RAMP SHALL BE OBTAINED BY COARSE BROOMING TRANSVERSE TO THE SLOPE OF THE RAMP.

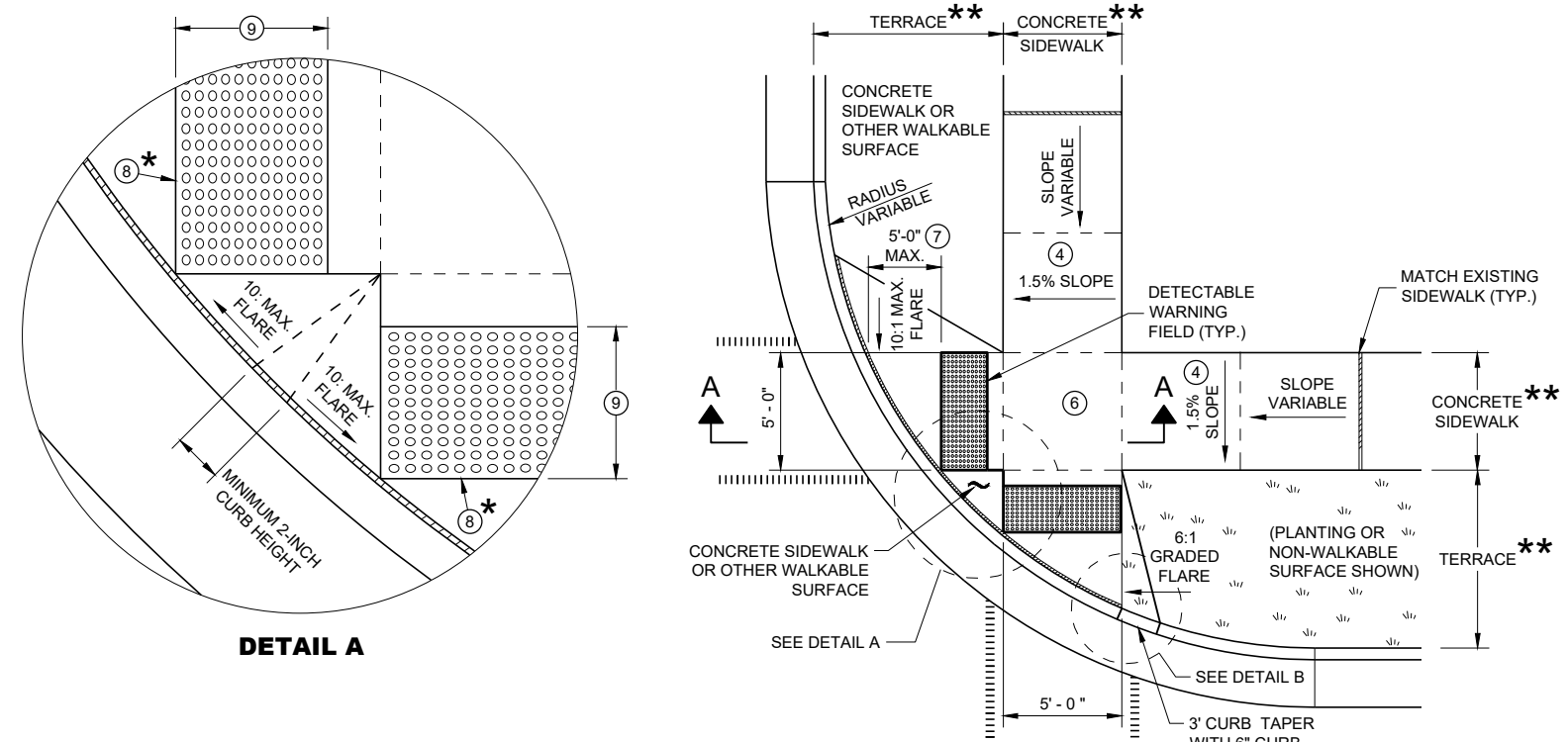
- ① THIS POINT IS AN EXTENSION OF OUTSIDE EDGE OF APPROACHING SIDEWALK WHERE IT MEETS THE BACK OF CONCRETE CURB. POINT LOCATION MAY BE ADJUSTED TO ALIGN WITH BEGINNING OF FULL-HEIGHT CURB IF THIS DISTANCE IS SHORT.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4" INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ MAXIMUM 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑤ PROVIDE A LEVEL LANDING IN THE STREET AND GUTTER AREA (2% MAXIMUM SLOPE IN ANY DIRECTION). WHEN THE GUTTER SLOPE EXCEEDS 2%, CONSTRUCT THE LEVEL LANDING IN THE STREET AREA.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

LEGEND

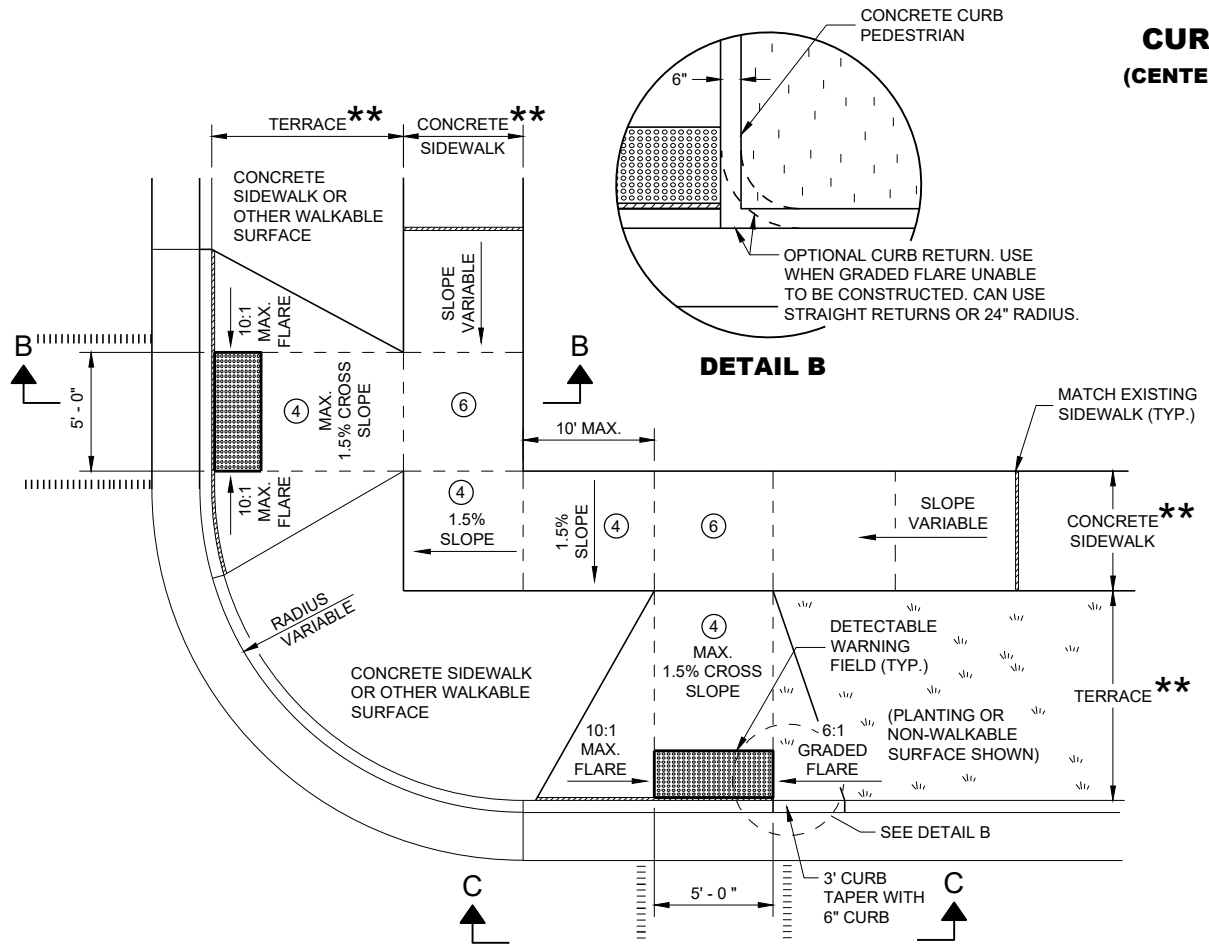
- 1/2" EXPANSION JOINT SIDEWALK
- - - - - CONTRACTION JOINT FIELD LOCATED
- ||||||| PAVEMENT MARKING CROSSWALK (WHITE)

**CURB RAMPS
TYPE 1 AND 1-A**

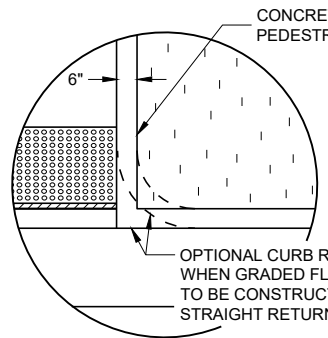
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PLAN VIEW CURB RAMP TYPE 2 (CENTER OF CORNER RADIUS)



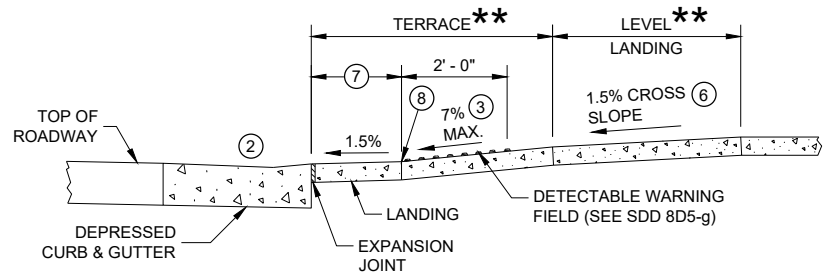
PLAN VIEW CURB RAMP TYPE 3 (OUTSIDE OF CROSSWALK AREA)



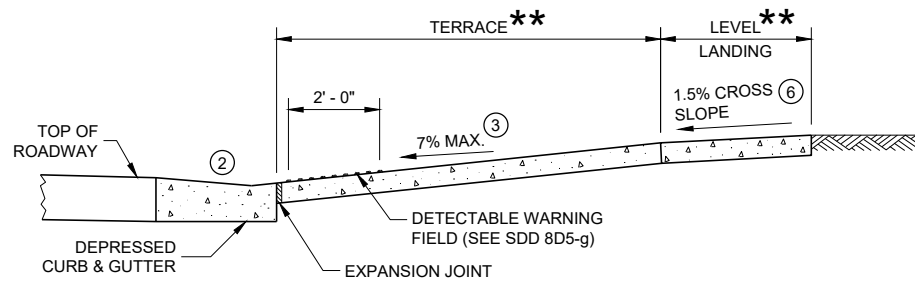
DETAIL B

GENERAL NOTES

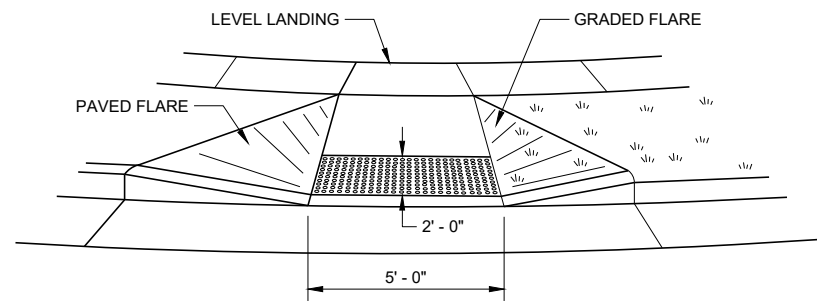
- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE (2.67% OR LESS) AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET X 5 FEET.
- ⑦ WHEN GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑨ WHEN DISTANCE IS LESS THAN 6' - 0", IT MAY BE DIFFICULT TO ACHIEVE A 7% SLOPE OR FLATTER ALONG THE RAMP. REDUCE CURB HEIGHT IN TRIANGLE AREA TO ACHIEVE 7% SLOPE OR FLATTER ON RAMP. CONSTRUCT 2-INCH MINIMUM CURB HEIGHT BETWEEN 10:1 FLARES.



SECTION A - A FOR TYPE 2



SECTION B - B FOR TYPE 3



VIEW C - C FOR TYPE 3

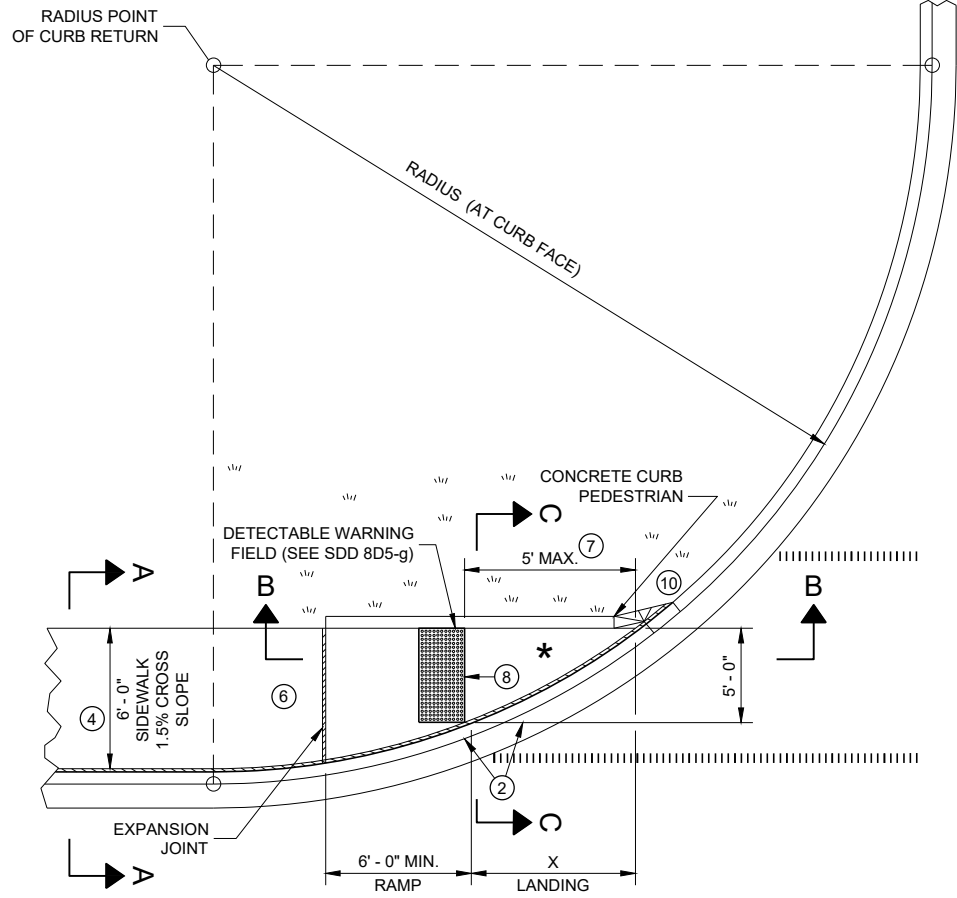
- * MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK
- ** WIDTH SHOWN ELSEWHERE IN THE PLANS

LEGEND

- 1/2" EXPANSION JOINT SIDEWALK
- - - CONTRACTION JOINT SIDEWALK
- ||||| PAVEMENT MARKING CROSSWALK (WHITE)

**CURB RAMPS
TYPE 2 AND 3**

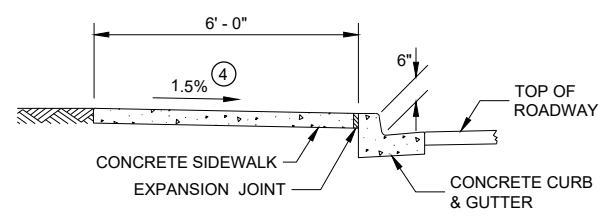
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PLAN VIEW CURB RAMP TYPE 4A

RADIUS (AT CURB FACE)	X
10 FEET	4' - 7"
15 FEET	6' - 5 1/2"

INTERMEDIATE RADII CAN BE INTERPOLATED



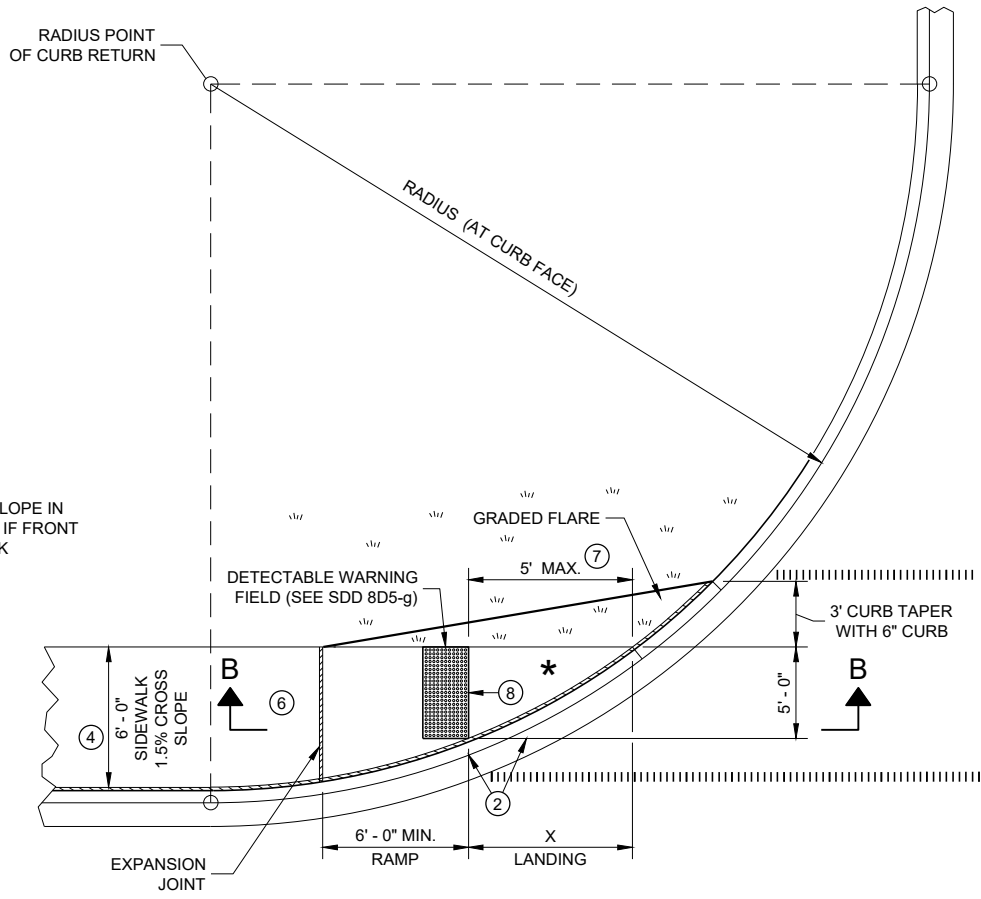
SECTION A - A FOR TYPE 4A

GENERAL NOTES

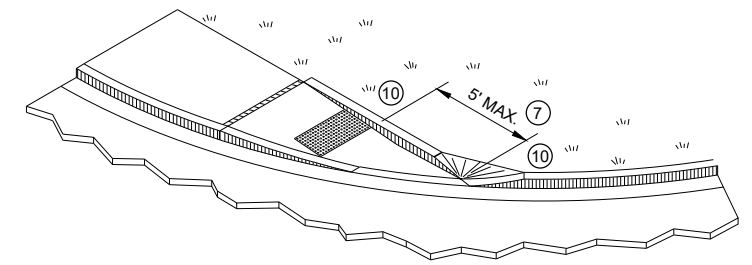
- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4" INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑦ WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑩ INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

LEGEND

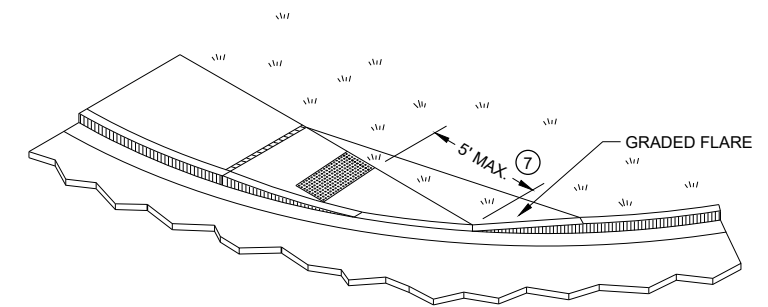
- 1/2" EXPANSION JOINT SIDEWALK
- - - CONTRACTION JOINT SIDEWALK
- ||||| PAVEMENT MARKING CROSSWALK (WHITE)



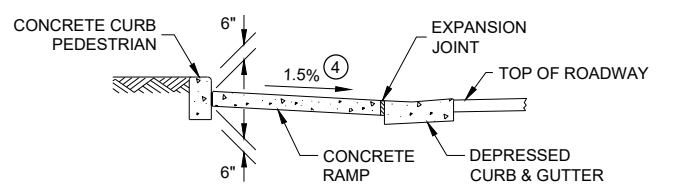
PLAN VIEW CURB RAMP TYPE 4A1



ISOMETRIC VIEW FOR TYPE 4A

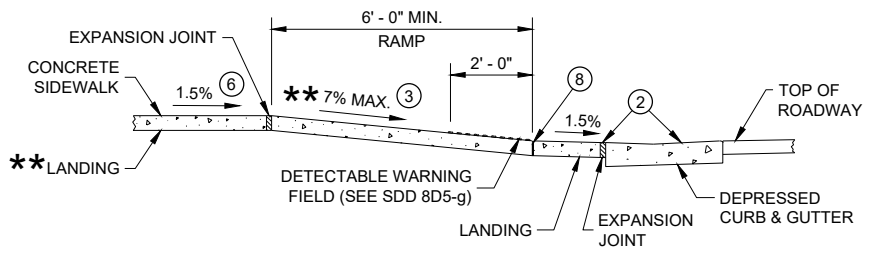


ISOMETRIC VIEW FOR TYPE 4A1



SECTION C - C FOR TYPE 4A

* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IF FRONT OF GRADE BREAK

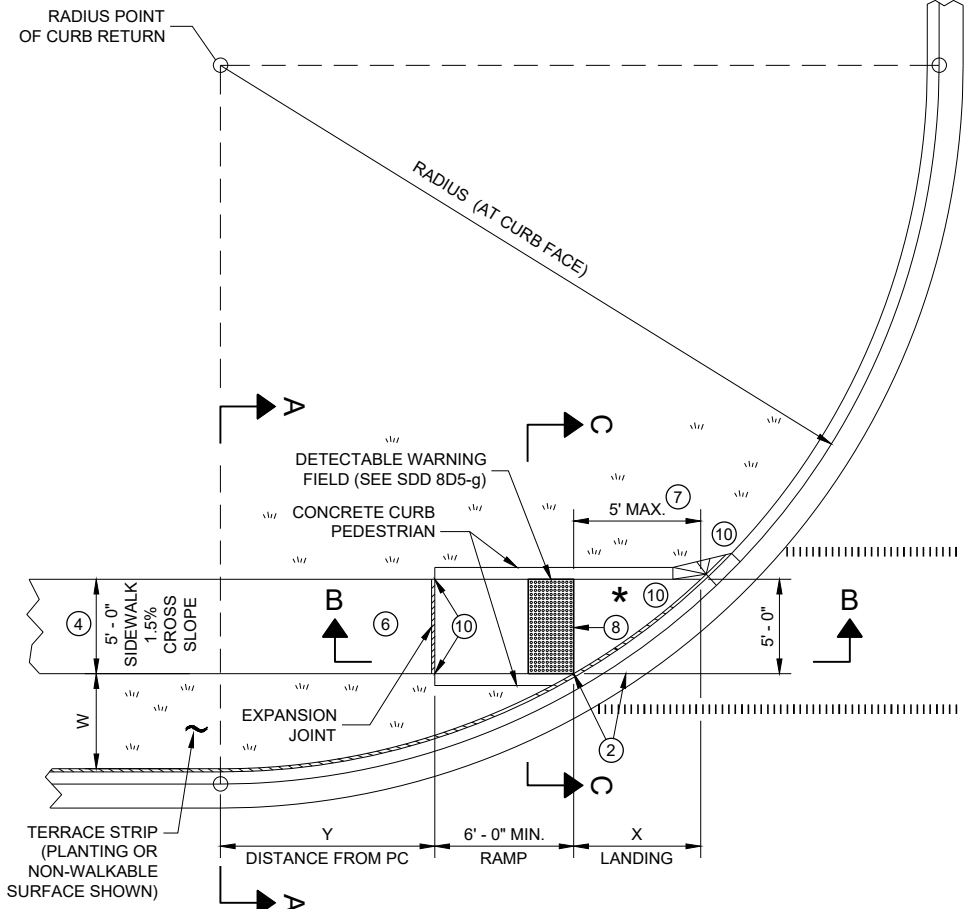


SECTION B - B FOR TYPE 4A AND TYPE 4A1

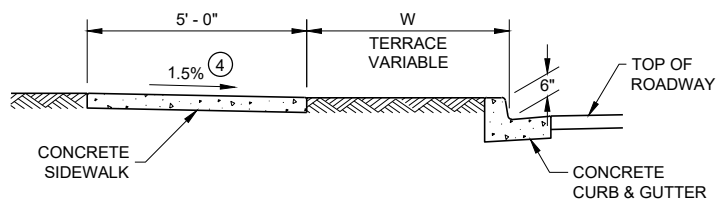
** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

**CURB RAMPS
TYPE 4A AND 4A1**

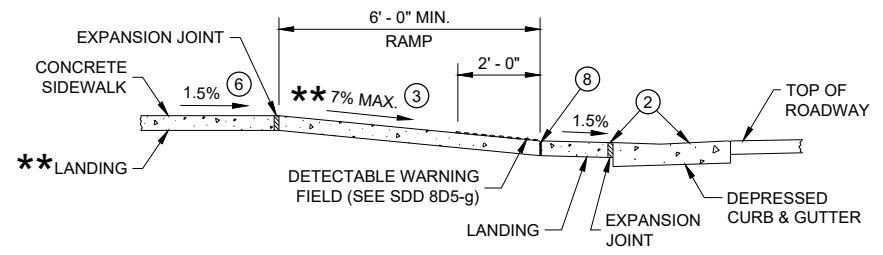
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



PLAN VIEW CURB RAMP TYPE 4B



SECTION A - A FOR TYPE 4B



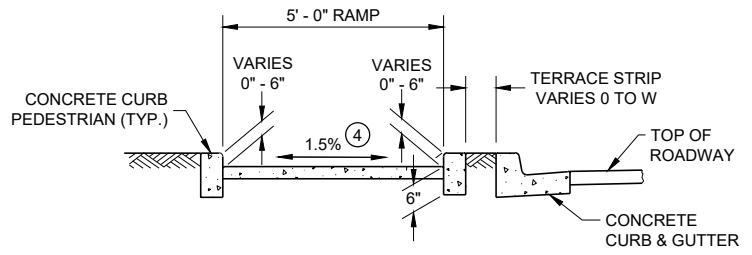
SECTION B - B FOR TYPE 4B AND TYPE 4B1

** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

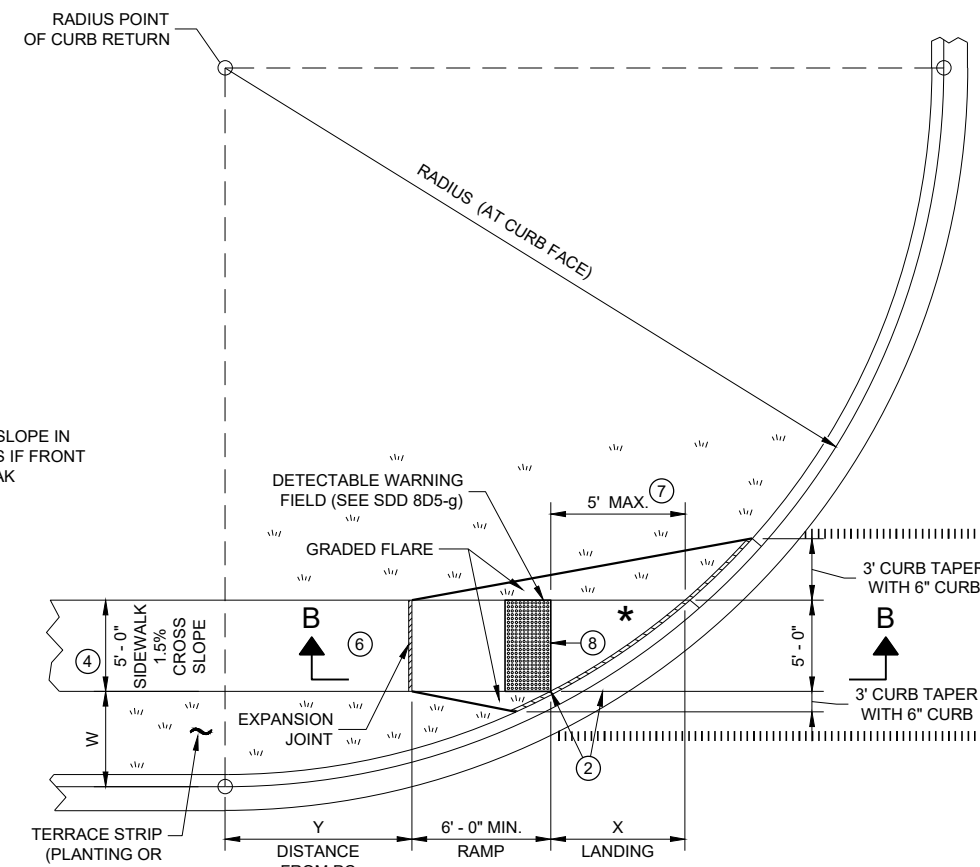
* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IF FRONT OF GRADE BREAK

RADIUS (AT CURB FACE)	W = 3' - 0"		W = 4' - 0"		W = 5' - 0"		W = 6' - 0"		W = 7' - 0"		W = 8' - 0"		W = 9' - 0"		W = 10' - 0"	
	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y
10 FEET	2' - 10 1/4"	0' - 5"	2' - 1"	1' - 4 1/2"	1' - 5"	2' - 1"	0' - 10"	2' - 7 1/2"	0' - 3 1/4"	3' - 0 1/4"						
15 FEET	4' - 6 3/4"	2' - 1 3/4"	3' - 9"	3' - 5 3/4"	3' - 1 1/4"	4' - 6"	2' - 6 3/4"	5' - 4 1/2"	2' - 1"	6' - 1"	1' - 8"	6' - 8 1/2"	1' - 3 1/4"	7' - 2 1/2"	0' - 10 3/4"	7' - 7 1/4"
20 FEET	5' - 9 3/4"	3' - 6 1/2"	4' - 11 1/2"	5' - 1 3/4"	4' - 3 1/4"	6' - 5 1/2"	3' - 8 3/4"	7' - 7"	3' - 3"	8' - 6 1/2"	2' - 10"	9' - 4 1/2"	2' - 5 1/2"	10' - 1 1/4"	2' - 1 1/4"	10' - 9"
30 FEET			6' - 9 1/4"	7' - 11 1/4"	6' - 0 1/4"	9' - 8"	5' - 5"	11' - 1 3/4"	4' - 10 3/4"	12' - 5 3/4"	4' - 5 1/2"	13' - 7 3/4"	4' - 0 3/4"	14' - 8 1/2"	3' - 8 1/2"	15' - 8 1/4"
40 FEET									6' - 1 3/4"	15' - 8 1/2"	5' - 8"	17' - 2"	5' - 3"	18' - 5 3/4"	4' - 10 3/4"	19' - 8 1/4"
50 FEET															5' - 10 1/4"	23' - 2"

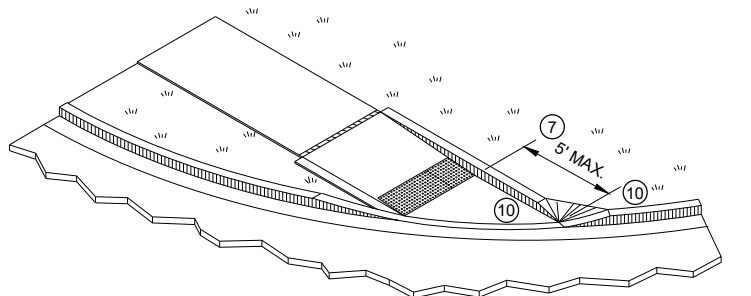
INTERMEDIATE RADII CAN BE INTERPOLATED
 DIMENSION "Y" IS CALCULATED BASED ON 6'-0" RAMP LENGTH
 DIMENSION "X" IS CALCULATED BASED ON 5'-0" SIDEWALK WIDTH



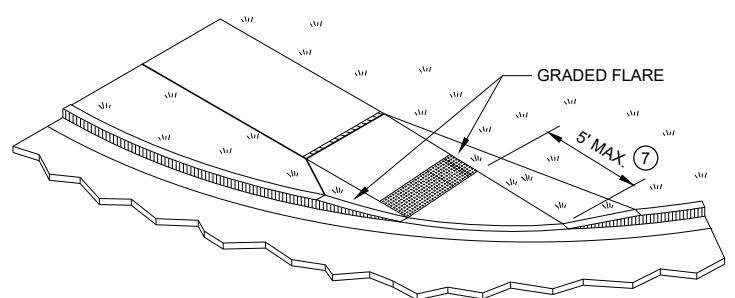
SECTION C - C FOR TYPE 4B



PLAN VIEW CURB RAMP TYPE 4B1



ISOMETRIC VIEW FOR TYPE 4B



ISOMETRIC VIEW FOR TYPE 4B1

LEGEND

- 1/2" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT SIDEWALK
- PAVEMENT MARKING CROSSWALK (WHITE)

GENERAL NOTES

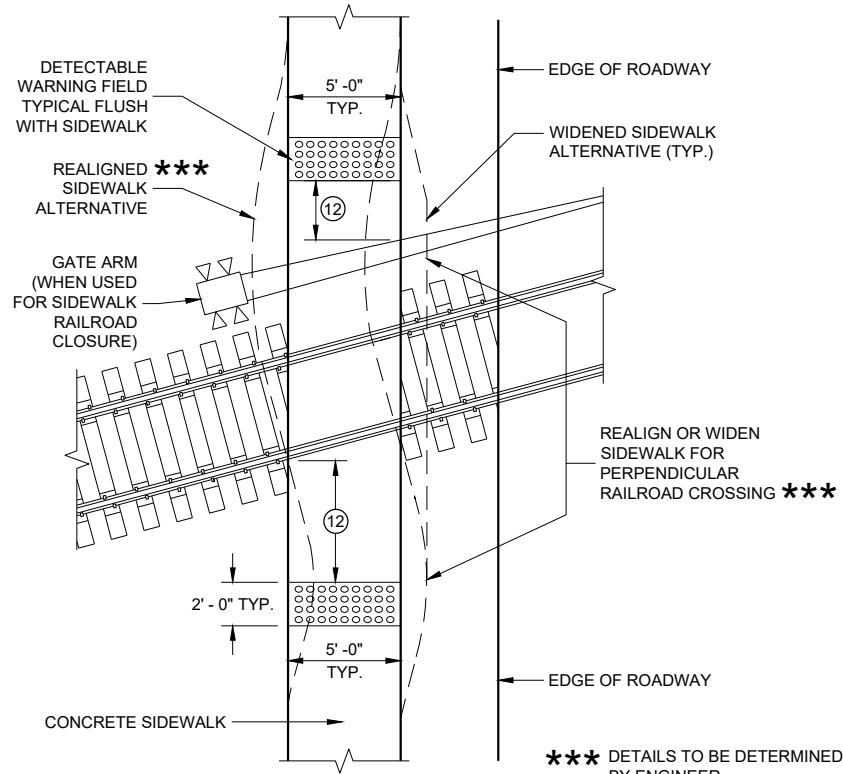
- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/2 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

SDD 08D05 - 20d

SDD 08D05 - 20d

CURB RAMPS TYPE 4B AND 4B1

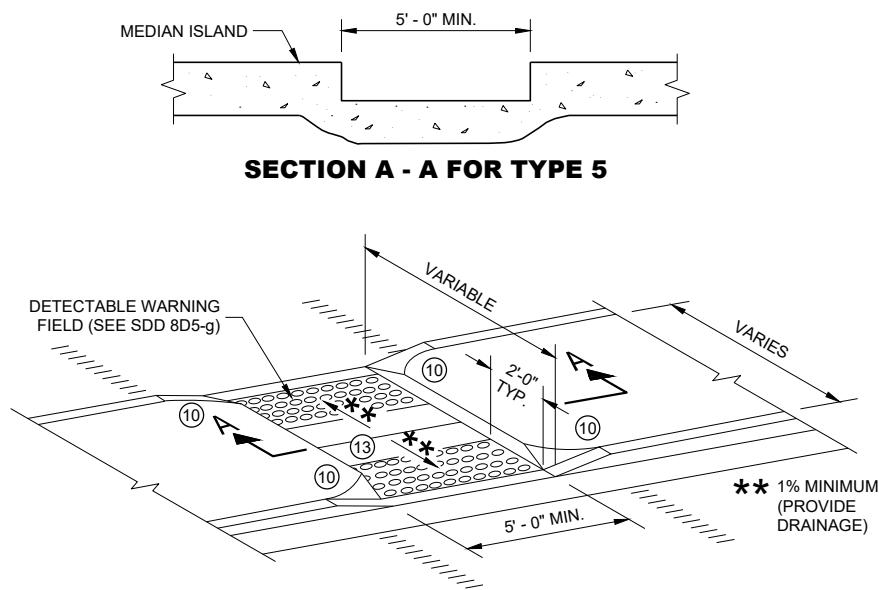
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION



CURB RAMP TYPE 8

DETECTABLE WARNINGS AT RAILROAD CROSSING

*** DETAILS TO BE DETERMINED BY ENGINEER



CURB RAMP TYPE 5
MEDIAN ISLAND
NON-ELEVATED PEDESTRIAN CROSSING

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

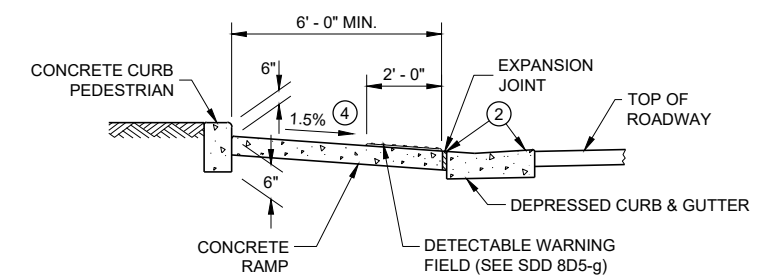
SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

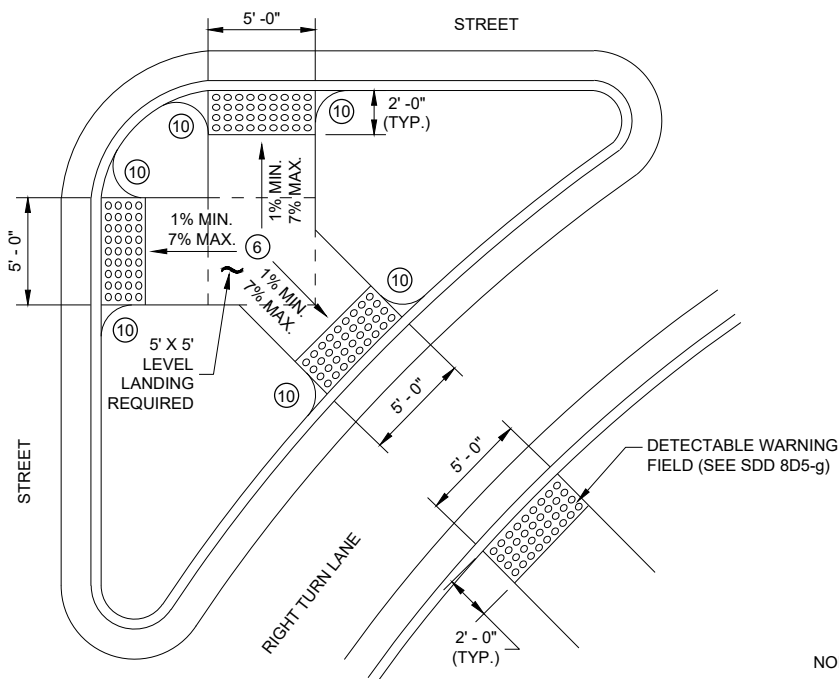
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑩ INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.
- ⑪ SLOPE SIDEWALK TOWARD LANDING AS SHOWN WHERE THERE IS NO TERRACE OR WHERE THE TERRACE WIDTH IS LESS THAN 6 FEET WIDE.
- ⑫ THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO A RAILROAD CROSSING SHALL BE 1.5 FEET ±0.1' FROM THE FACE OF THE GATE ARM IF THE GATE ARM EXTENDS ACROSS THE SIDEWALK. WHERE THERE IS NO PEDESTRIAN GATE, THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO THE RAILROAD CROSSING SHALL BE 15 FEET FROM THE NEAREST RAIL.
- ⑬ DO NOT INSTALL DETECTABLE WARNING FIELDS AT THE EDGES OF STREET-LEVEL PEDESTRIAN REFUGE ISLANDS IF A MINIMUM 2 FOOT CONCRETE SURFACE WITHOUT DETECTABLE WARNINGS (MEASURED IN THE DIRECTION OF PEDESTRIAN TRAVEL) CANNOT BE ACHIEVED.

LEGEND

- ===== 1/2" EXPANSION JOINT SIDEWALK
- - - - - CONTRACTION JOINT FIELD LOCATED
- ||||||| PAVEMENT MARKING CROSSWALK (WHITE)

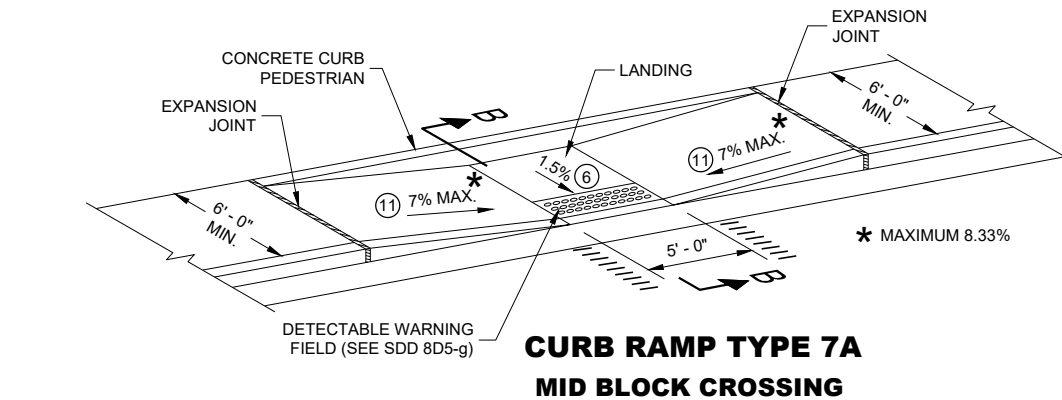


SECTION B - B FOR TYPE 7A

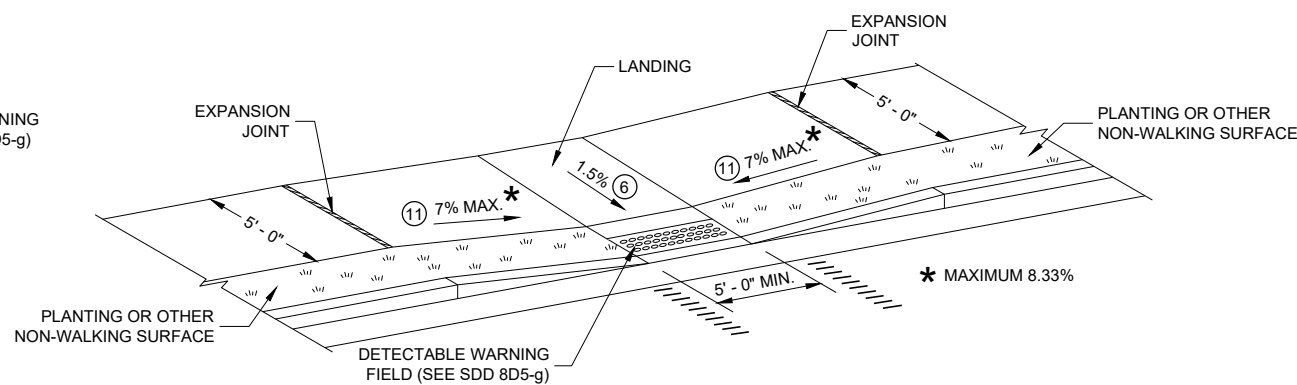


CURB RAMP TYPE 6
DETECTABLE WARNING AT ISLANDS

REFER TO GENERAL NOTES ② AND ③ FOR ALL ISLAND CURB RAMPS



CURB RAMP TYPE 7A
MID BLOCK CROSSING



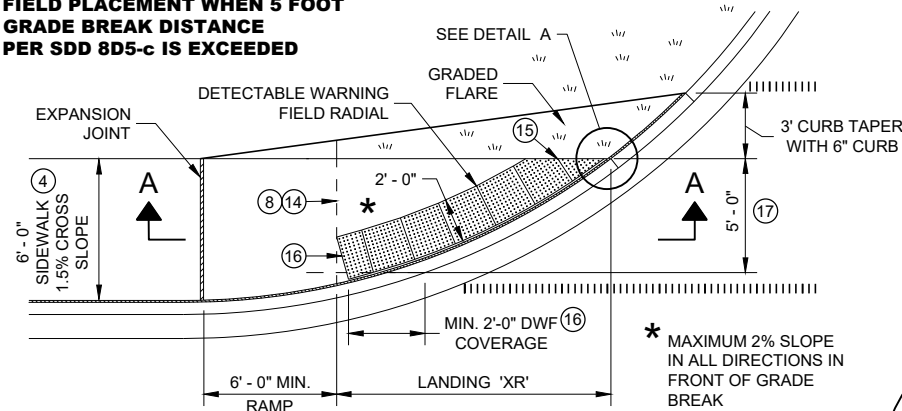
CURB RAMP TYPE 7B
MID BLOCK CROSSING

NOTE: THESE PARALLEL AND PARALLEL/PERPENDICULAR CURB RAMPS MAY BE USED AT INTERSECTIONS AND MID BLOCK LOCATIONS.

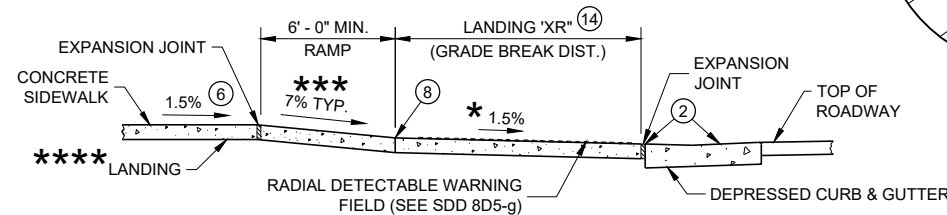
CURB RAMPS
TYPE 5, 6, 7A, 7B & 8

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-c IS EXCEEDED



PLAN VIEW CURB RAMP TYPE 4A1 (GRADE BREAK DISTANCE GREATER THAN 5 FEET)

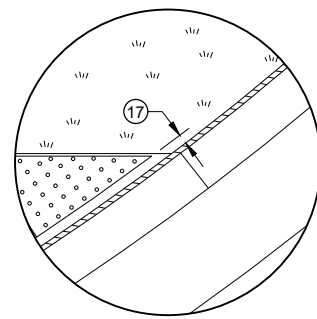


SECTION A - A FOR TYPE 4A1

**** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

*** MAXIMUM 8.33%

- LEGEND**
- 1/2" EXPANSION JOINT SIDEWALK
 - - - - - CONTRACTION JOINT SIDEWALK
 - ||||| PAVEMENT MARKING CROSSWALK (WHITE)



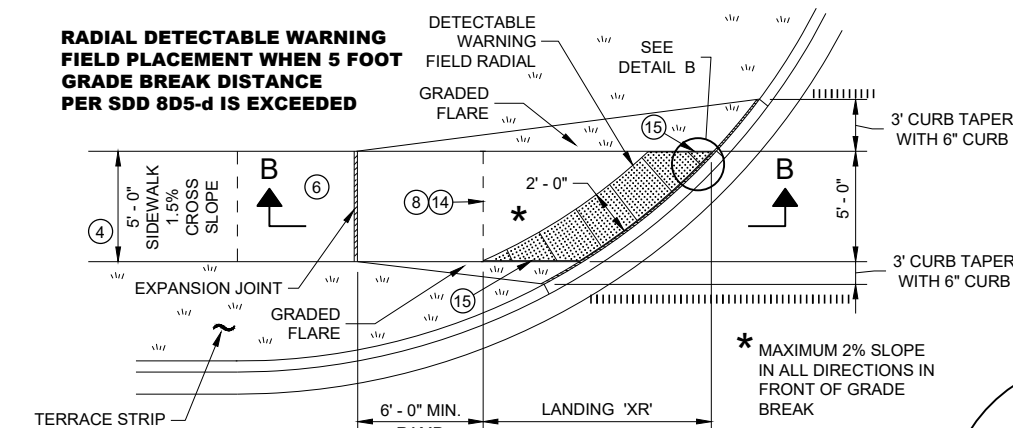
DETAIL A

GENERAL NOTES

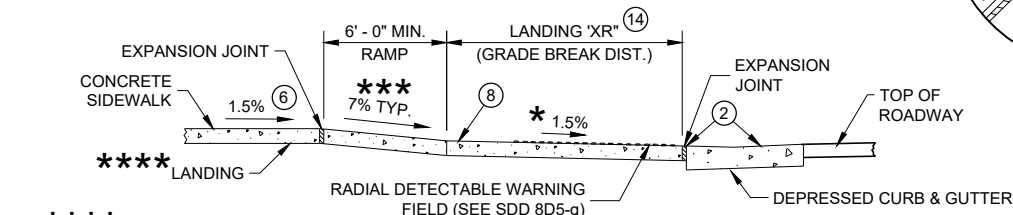
- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- APPLY RADIAL DETECTABLE WARNING PLACEMENT SIMILARLY FOR TYPE 4A AND 4A1 CURB RAMPS AND SIMILARLY FOR TYPE 4B AND 4B1 CURB RAMPS. TYPE 4A AND 4B RAMPS ARE NOT SHOWN.
- REFER TO SDD 8D5-g FOR ADDITIONAL RADIAL PLATE REQUIREMENTS.
- FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FILED ARE PROHIBITED.
- DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.
- 2 GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
 - 3 AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
 - 4 ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
 - 6 PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET BY 5 FEET.
 - 8 PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
 - 14 CONSULT ENGINEER IF GRADE BREAK LOCATION (END OF LANDING DIMENSION "XR") REQUIRES FIELD ADJUSTMENT WHEN ESTABLISHING FINAL RADIAL DETECTABLE WARNING FIELD LOCATION.
 - 15 FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN 1/8" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.
 - 16 USE 1' X 2" RECTANGULAR END PLATE AT END OF TYPE 4A1 RAMP AND PROVIDE MINIMUM 2' - 0" DETECTABLE WARNING FIELD COVERAGE (IN DIRECTION OF PEDESTRIAN TRAVEL) ALONG THE ENTIRE CURB RAMP WIDTH.
 - 17 A MAXIMUM 3 INCH CONCRETE BORDER WITH IS ALLOWABLE IN FROM OF RADIAL DETECTABLE WARNING FIELD FOR CONSTRUCTABILITY PURPOSES. CONCRETE BORDER WIDTH MAY VARY UP TO 1 INCH.

6

RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-d IS EXCEEDED



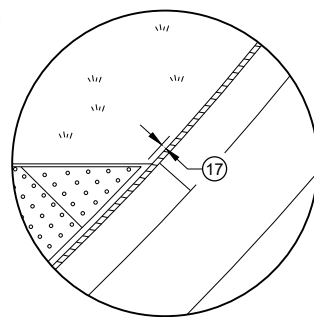
PLAN VIEW CURB RAMP TYPE 4B1 (GRADE BREAK DISTANCE GREATER THAN 5 FEET)



SECTION B - B FOR TYPE 4B1

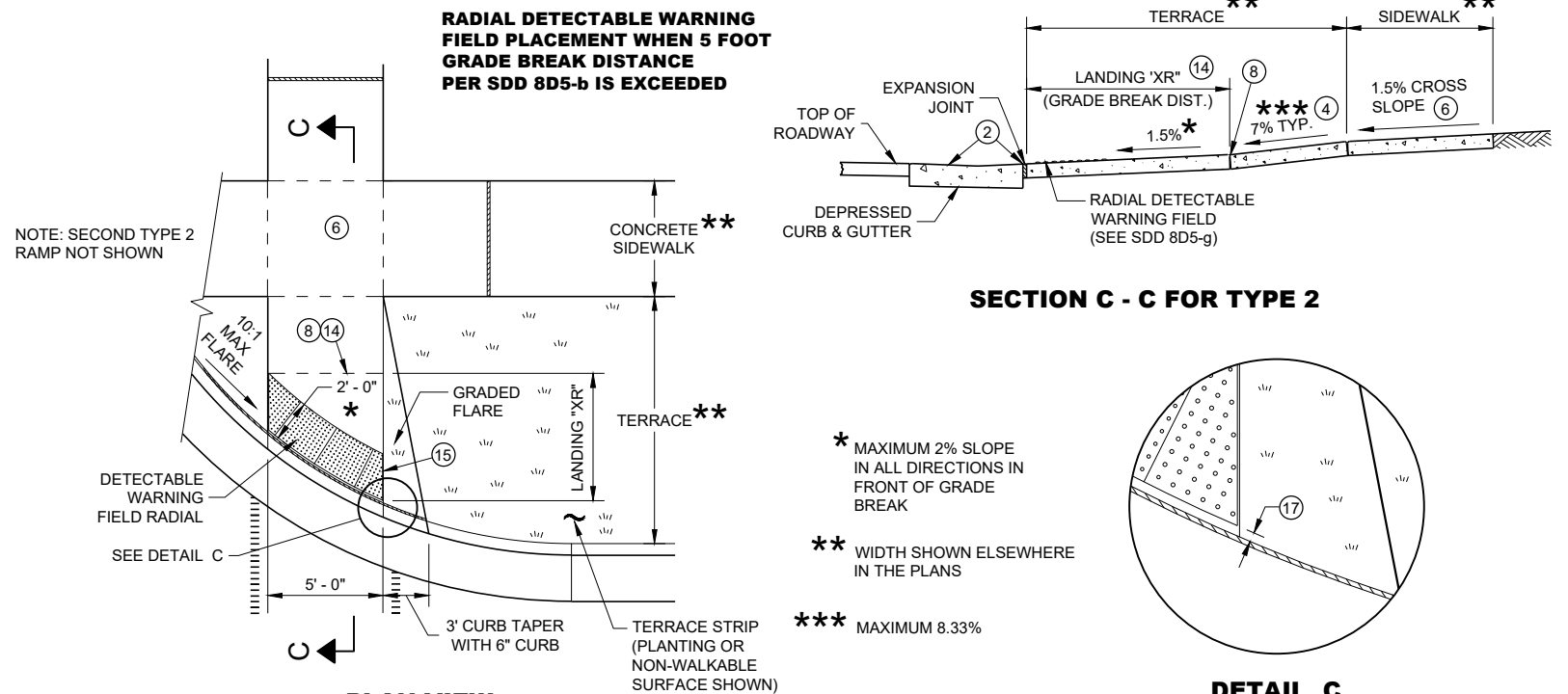
**** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

*** MAXIMUM 8.33%



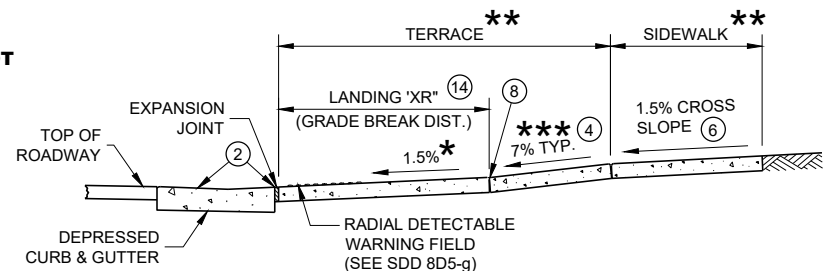
DETAIL B

RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-b IS EXCEEDED



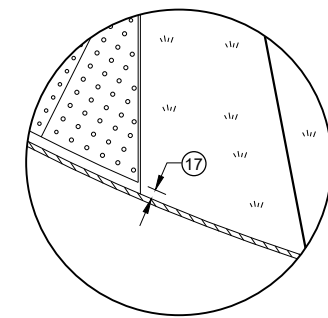
PLAN VIEW CURB RAMP TYPE 2 (GRADE BREAK DISTANCE GREATER THAN 5 FEET) (ON LINE WITH SIDEWALK)

NOTE: SECOND TYPE 2 RAMP NOT SHOWN



SECTION C - C FOR TYPE 2

- * MAXIMUM 2% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK
- ** WIDTH SHOWN ELSEWHERE IN THE PLANS
- *** MAXIMUM 8.33%



DETAIL C

CURB RAMPS RADIAL DETECTABLE WARNING FIELD APPLICATIONS

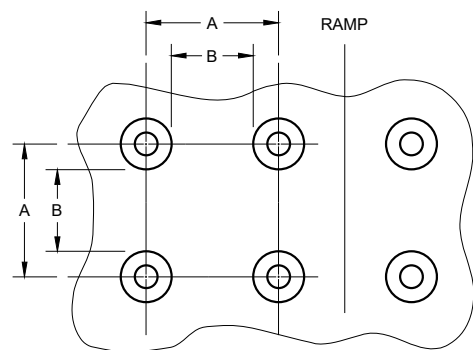
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

SDD 08D05 - 20f

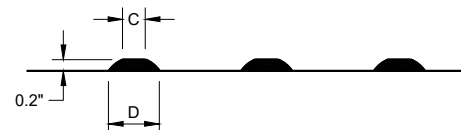
SDD 08D05 - 20f

	MIN.	MAX.
A	1.6"	2.4"
B	0.65"	1.5"
C	*	*
D	0.9"	1.4"

* THE C DIMENSION IS 50% TO 65% OF THE D DIMENSION.

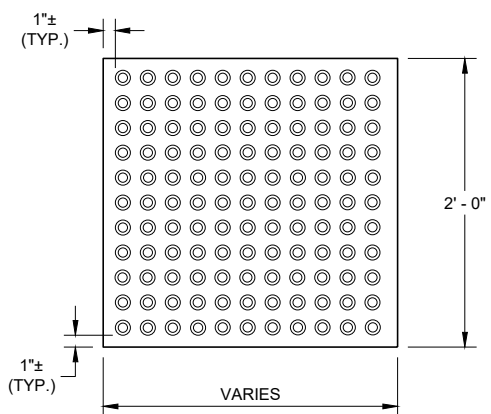


PLAN VIEW

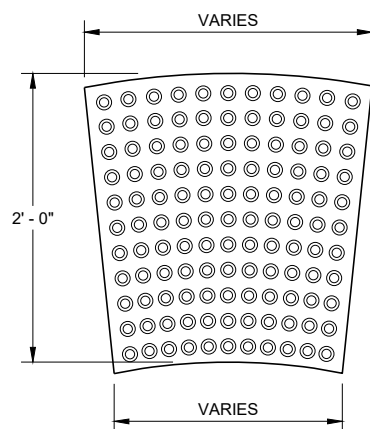


ELEVATION VIEW

**TRUNCATED DOMES
DETECTABLE WARNING PATTERN DETAIL**

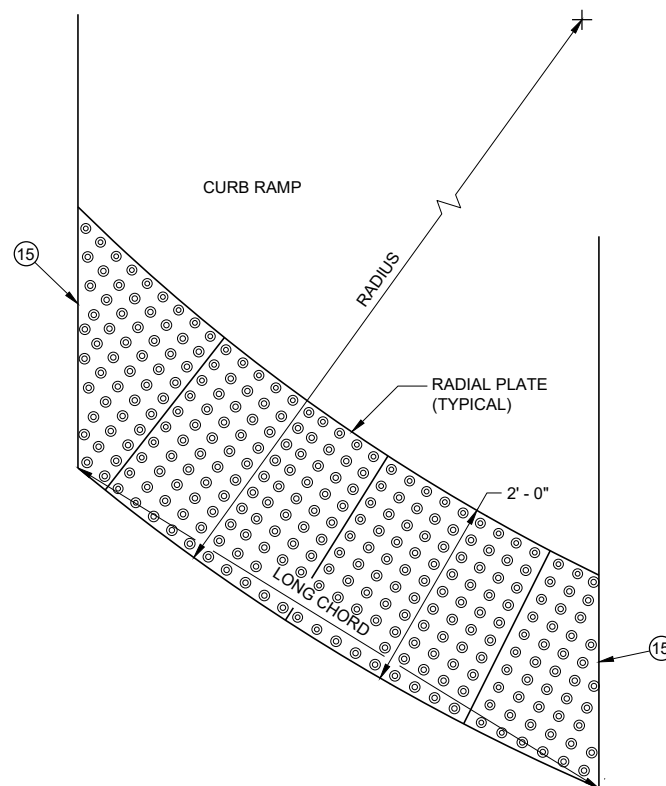


RECTANGULAR
PLATES

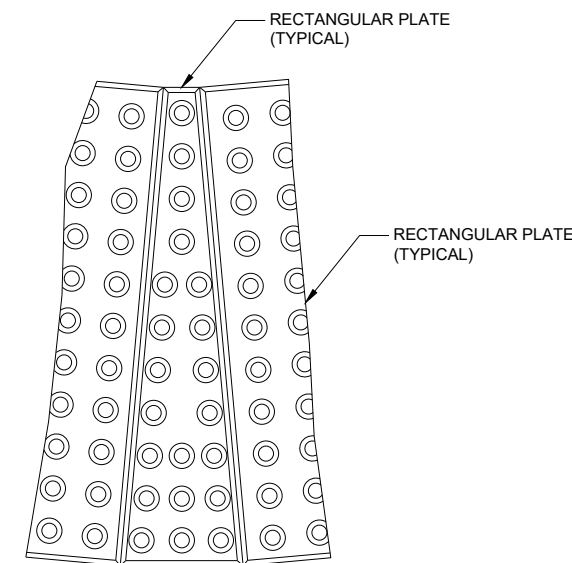


RADIAL
PLATES

PLAN VIEW
DETECTABLE WARNING FIELDS (TYPICAL)



PLAN VIEW
RADIAL DETECTABLE
WARNING FIELD ATTRIBUTES



PLAN VIEW
RADIAL WEDGE PLATE
CONNECTION DETAIL

GENERAL NOTES

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AT A CURB RAMP SHALL BE FROM THE SAME MANUFACTURER.

PLACE ALL DETECTABLE WARNING FIELD SYSTEMS IN ACCORDANCE TO THE MANUFACTURER'S RECOMMENDATION.

FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FIELD ARE PROHIBITED.

DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.

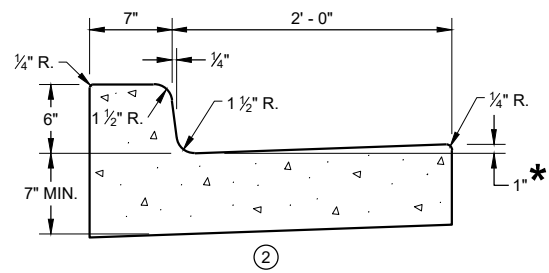
FOR RADIAL DETECTABLE WARNING FIELD APPLICATIONS WHERE STANDARD RADIAL PLATES ARE NOT AVAILABLE AT AN INTERSECTION CURB RADIUS, A COMBINATION OF SQUARE OR RECTANGULAR PLATES AND RADIAL PLATES MAY BE USED TO FORM RADIAL CONFIGURATION. RADIAL WEDGE PLATES IN COMBINATION WITH SQUARE PLATES ARE ALSO ACCEPTABLE. FOLLOW MANUFACTURER'S RECOMMENDATIONS.

REFER TO CONTRACT AND STANDARD SPECIFICATIONS FOR FIELD CUTTING REQUIREMENTS.

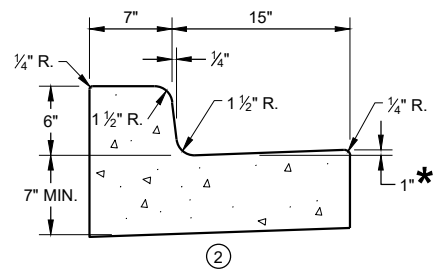
DO NOT EMBED IN CONCRETE ANY FIELD-CUT PLATES WITH CUT EDGES SHORTER THAN 6 INCHES. CONSULT WITH MANUFACTURER FOR RE-DRILLING AND ANCHORING REQUIREMENTS OF FIELD-CUT PLATES.

15 FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN 1/8" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.

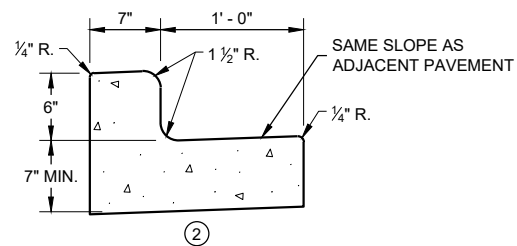
CURB RAMPS RECTANGULAR AND RADIAL DETECTABLE WARNING PLATES	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2019 DATE	/S/ Rodney Taylor ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR
<small>FHWA</small>	



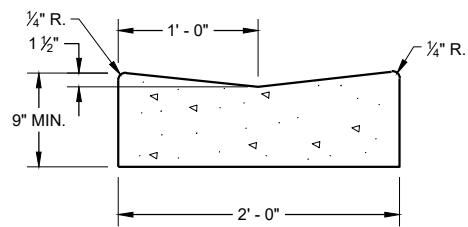
CONCRETE CURB AND GUTTER 31" ①



CONCRETE CURB AND GUTTER 22" ①

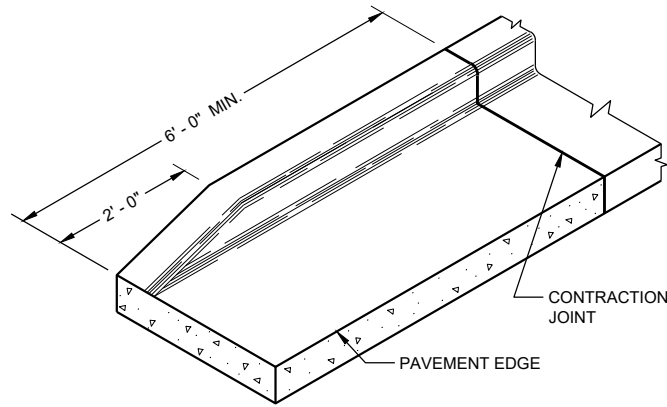


CONCRETE CURB AND GUTTER 19" ①

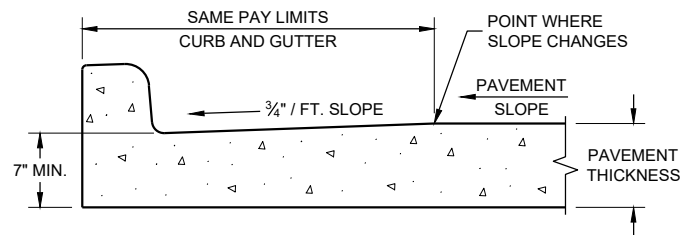


CONCRETE GUTTER 24" ①

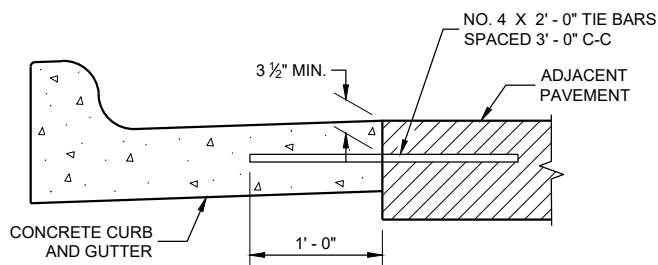
* TO BE MEASURED TO A MAXIMUM OF 3" WHERE DRAINAGE PROBLEMS EXIST.



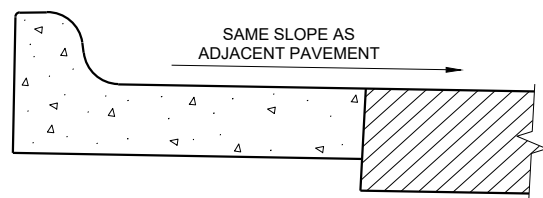
END SECTION CURB AND GUTTER



PARTIAL SECTION OF PAVEMENT WITH INTEGRAL CURB AND GUTTER



TYPICAL TIE BAR LOCATION ①



HIGH SIDE SECTION ③
(TYPICAL FOR ALL CURB & GUTTER TYPES)

GENERAL NOTES

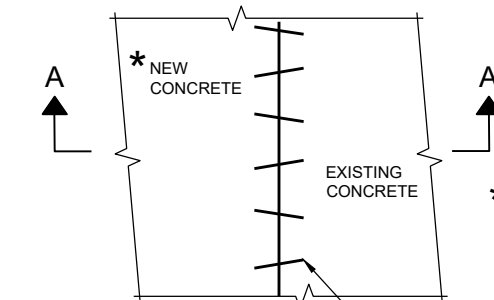
DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

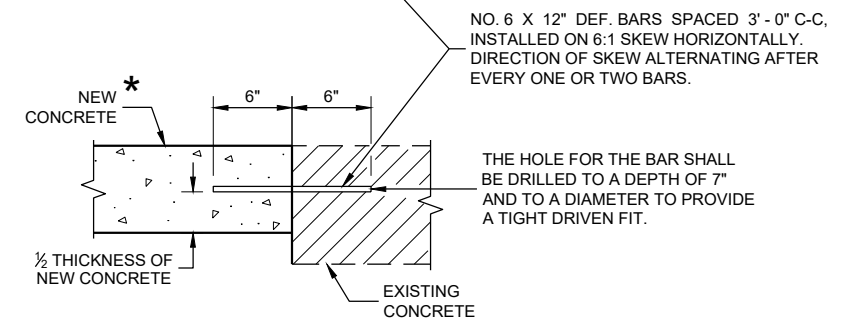
INTEGRAL CURB AND GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB AND GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE. A LONGITUDINAL CONSTRUCTION JOINT IS NOT REQUIRED WITH INTEGRAL CURB AND GUTTER.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE COURSE AND UNCLASSIFIED EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

- ① WHEN PLACED ADJACENT TO NEW CONCRETE, TIE BARS ARE REQUIRED FOR CURB AND GUTTER 31", 22", 19" AND CONCRETE GUTTER 24".
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE COURSE PROVIDED A 7" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ③ WHEN HIGH SIDE CURB SECTION IS REQUIRED, THE LOCATION(S) WILL BE NOTED ON THE PLANS



PLAN VIEW



**SECTION A - A
PAVEMENT TIES**

**CONCRETE GUTTER,
CURB AND GUTTER AND
PAVEMENT TIES**
(For Optional use in Milwaukee Co. Only)

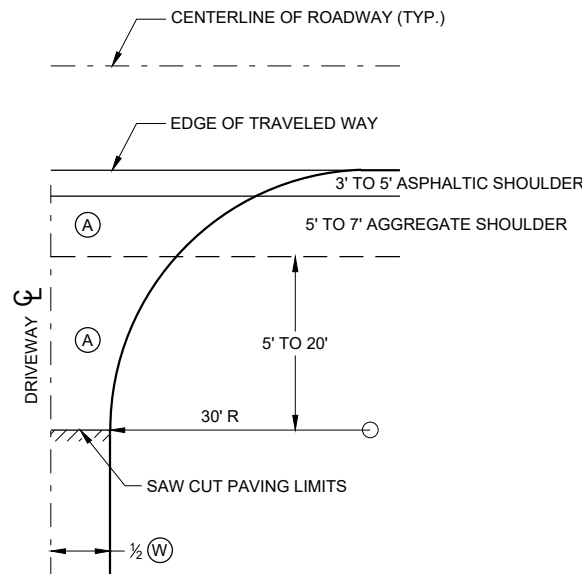
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
ENGINEER

FHWA

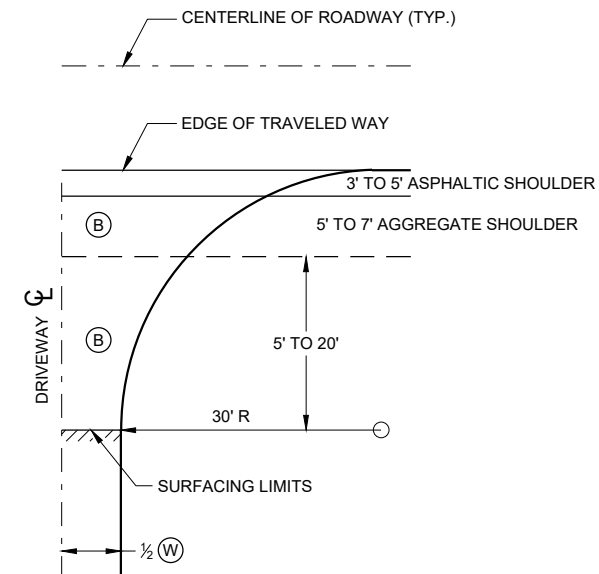
GENERAL NOTES

- ① DESIGN WILL DETERMINE FINAL DRIVEWAY ASPHALTIC THICKNESS BASED ON TYPE OF USAGE AND LOADINGS.

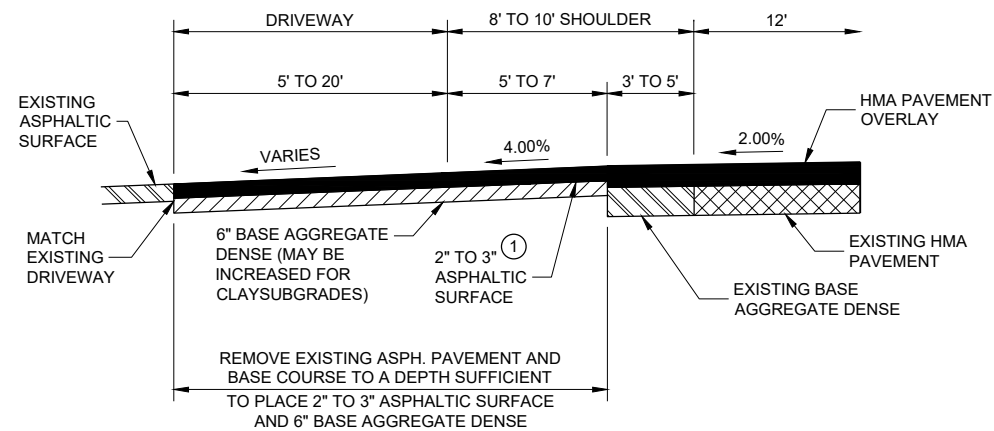


- Ⓐ : PAID FOR AS ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES. (TON)
- Ⓑ : PAID FOR AS BASE AGGREGATE DENSE 1 1/4" (TON)
- ⒲ : DRIVEWAY WIDTH 16' MIN. - 24' MAX.

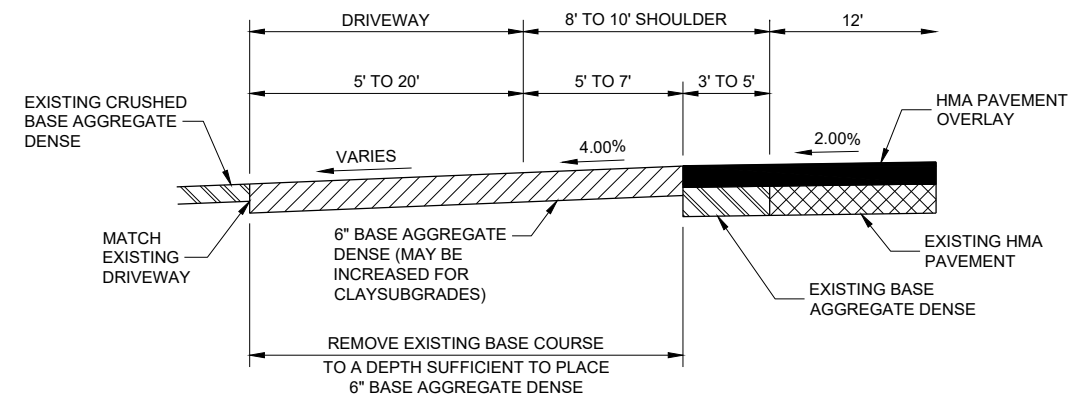
**PLAN VIEW
HALF SECTION**



**PLAN VIEW
HALF SECTION**



**PROFILE VIEW
RURAL ENTRANCE
WITH ASPHALTIC SURFACE
RESURFACING PROJECTS**



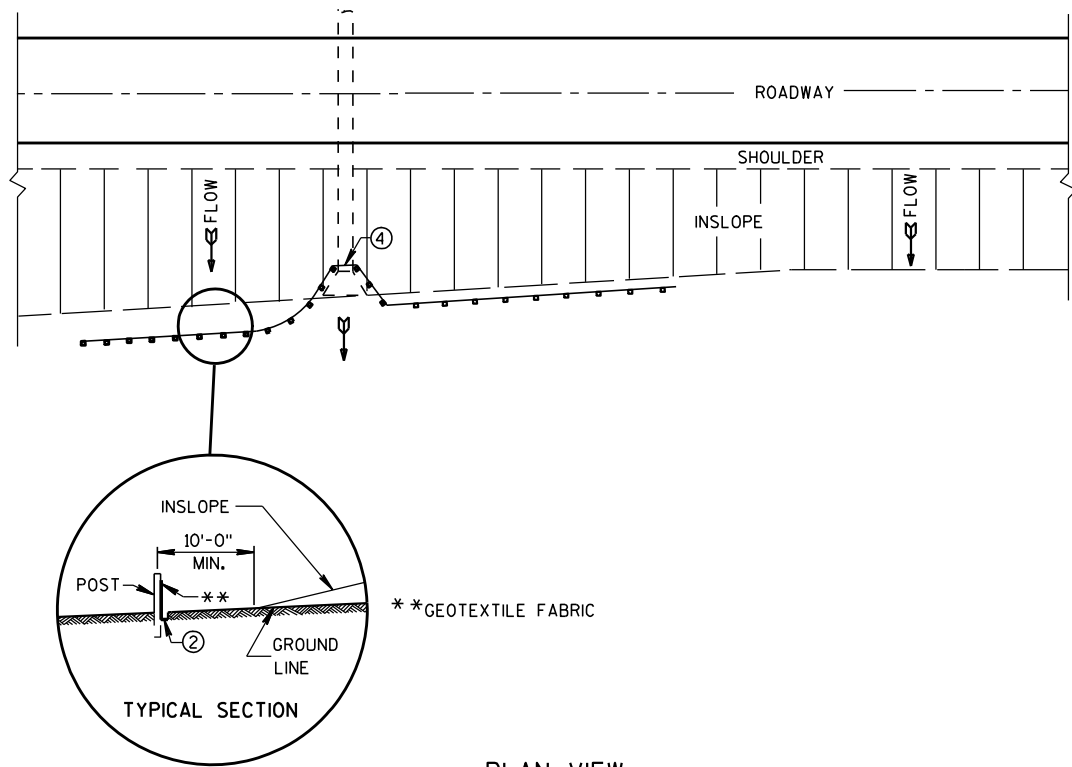
**PROFILE VIEW
RURAL ENTRANCE
WITH AGGREGATE SURFACE
6" BASE AGGREGATE DENSE
RESURFACING PROJECTS**

**DRIVEWAYS WITHOUT CURB
AND GUTTER RESURFACING
PROJECTS RURAL**

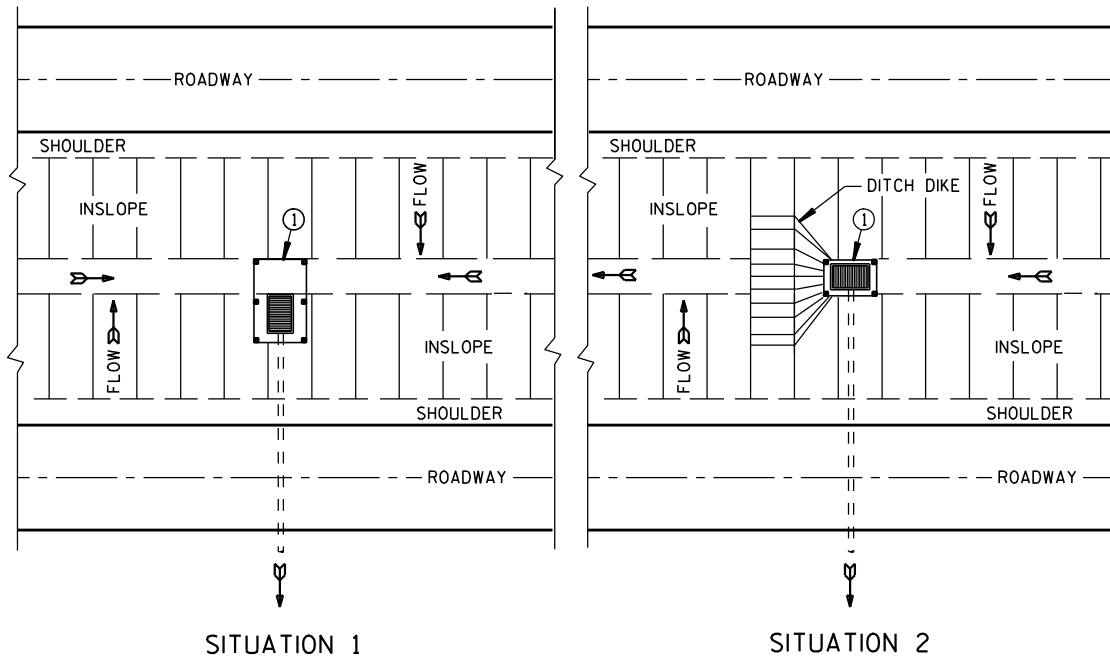
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
December 2016 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
ENGINEER

FHWA



PLAN VIEW
TYPICAL APPLICATION OF SILT FENCE

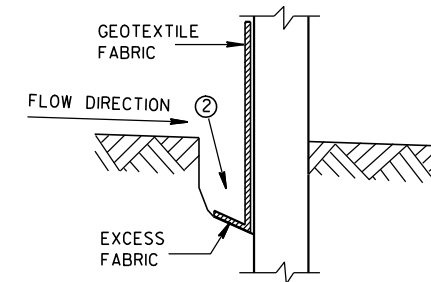


SITUATION 1 SITUATION 2
PLAN VIEW
SILT FENCE AT MEDIAN SURFACE DRAINS

GENERAL NOTES

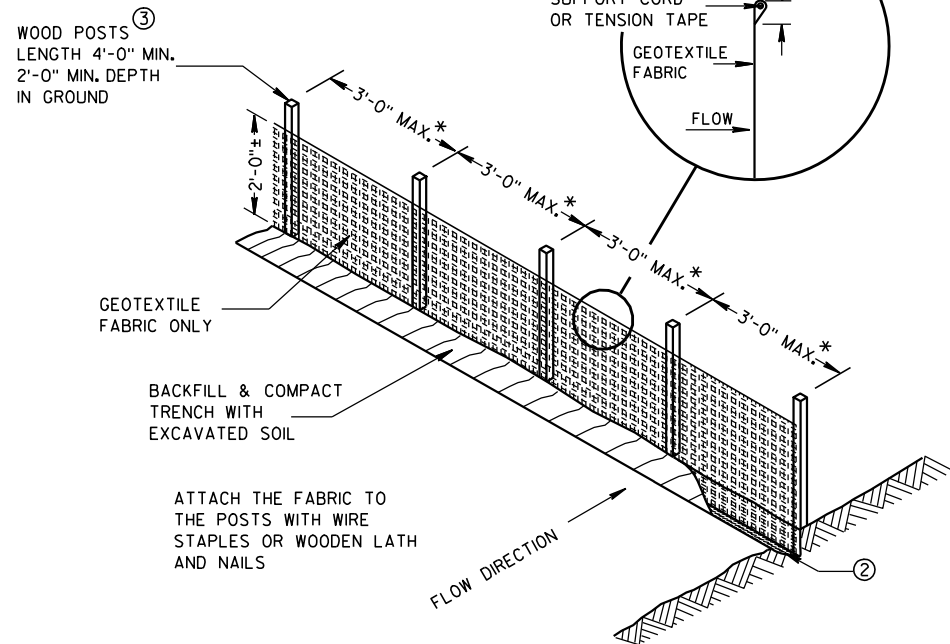
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

- ① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- ③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1 1/8" X 1 1/8" OF OAK OR HICKORY.
- ④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- ⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



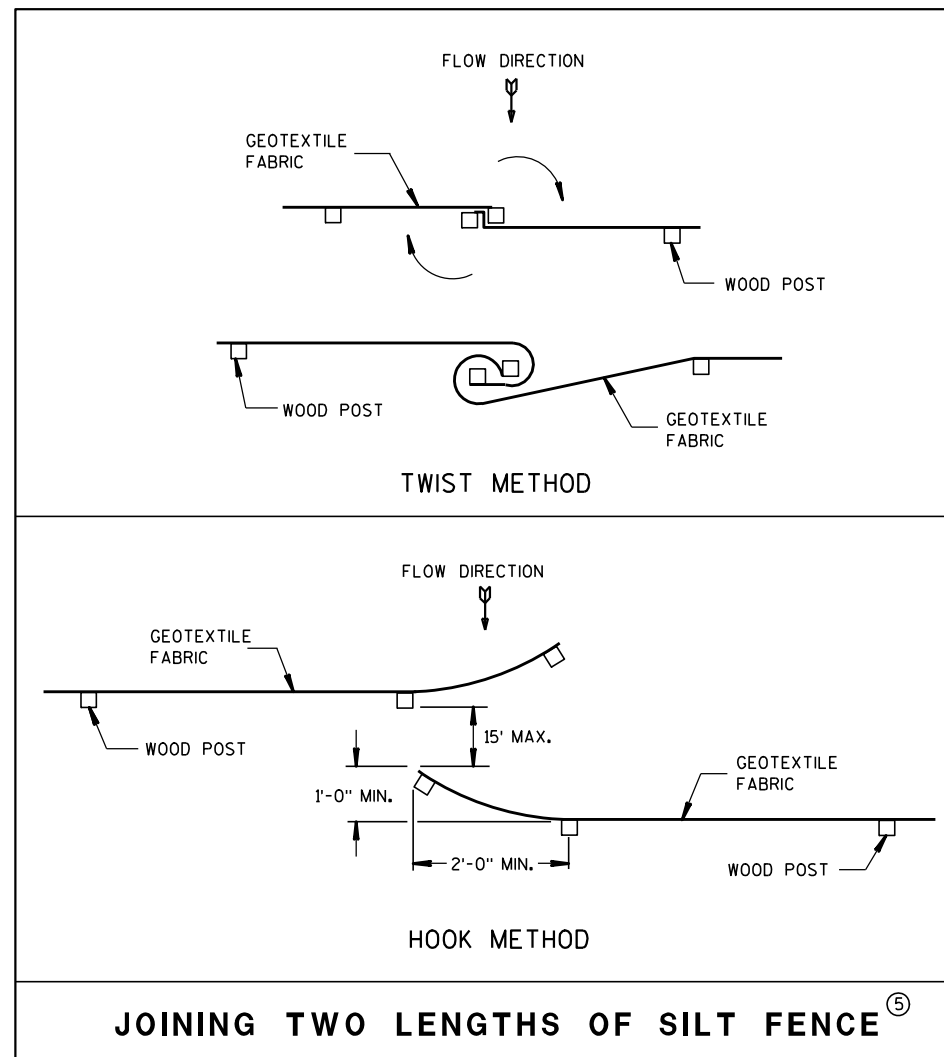
TRENCH DETAIL

NOTE: ADDITIONAL POST DEPTH OR TIE BACKS MAY BE REQUIRED IN UNSTABLE SOILS

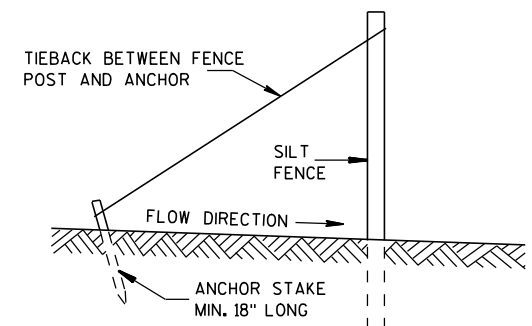


SILT FENCE

* NOTE: 8'-0" POST SPACING ALLOWED IF A WOVEN GEOTEXTILE FABRIC IS USED.



JOINING TWO LENGTHS OF SILT FENCE ⑤

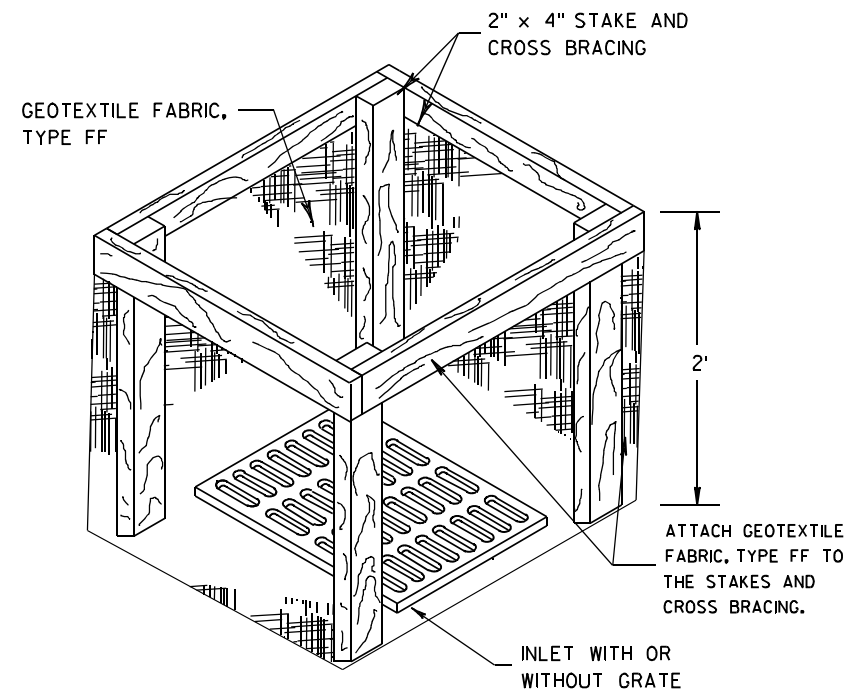
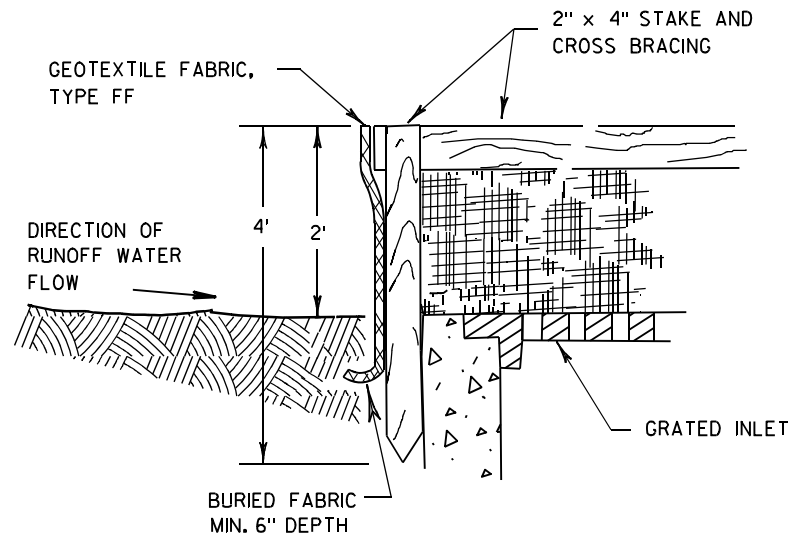


SILT FENCE TIE BACK
(WHEN REQUIRED BY THE ENGINEER)

SILT FENCE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
4-29-05 /S/ Beth Cannestra
DATE CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA



INLET PROTECTION, TYPE A

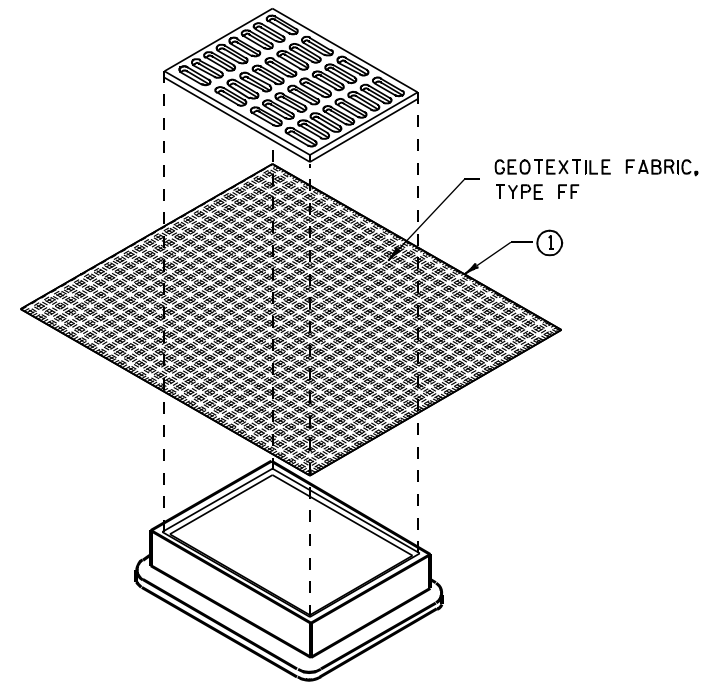
GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

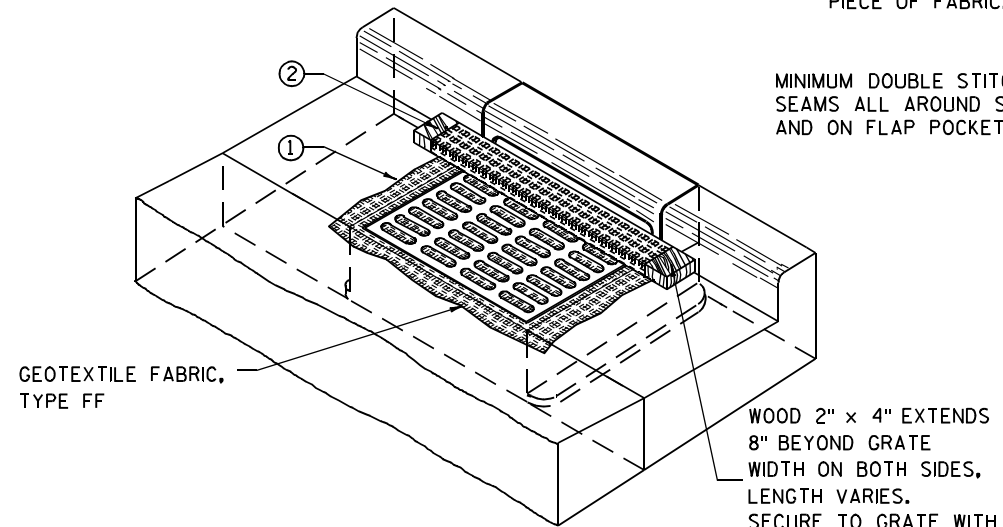
WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- ① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- ② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- ③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



**INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)**

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE C (WITH CURB BOX)

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

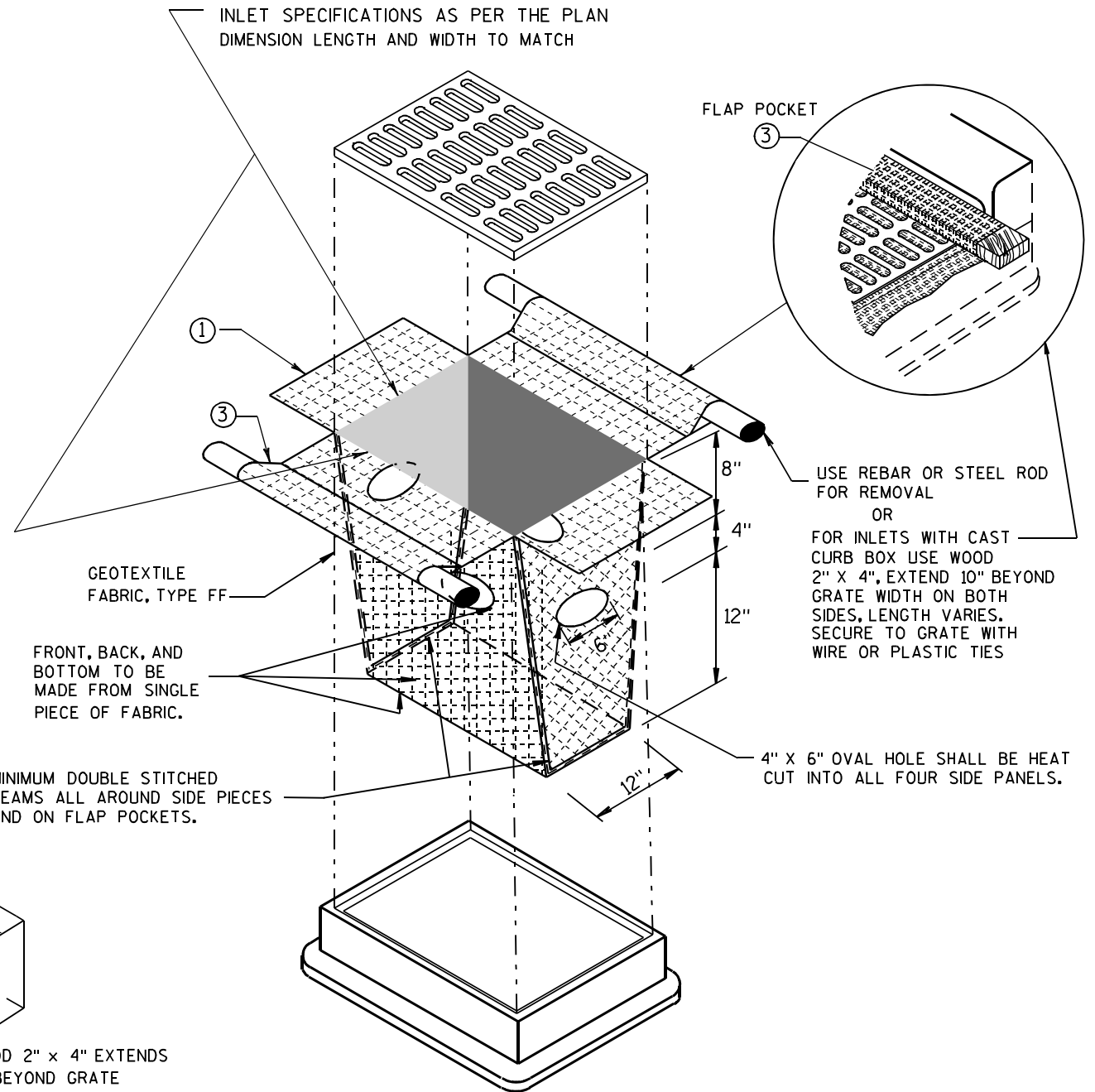
THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.



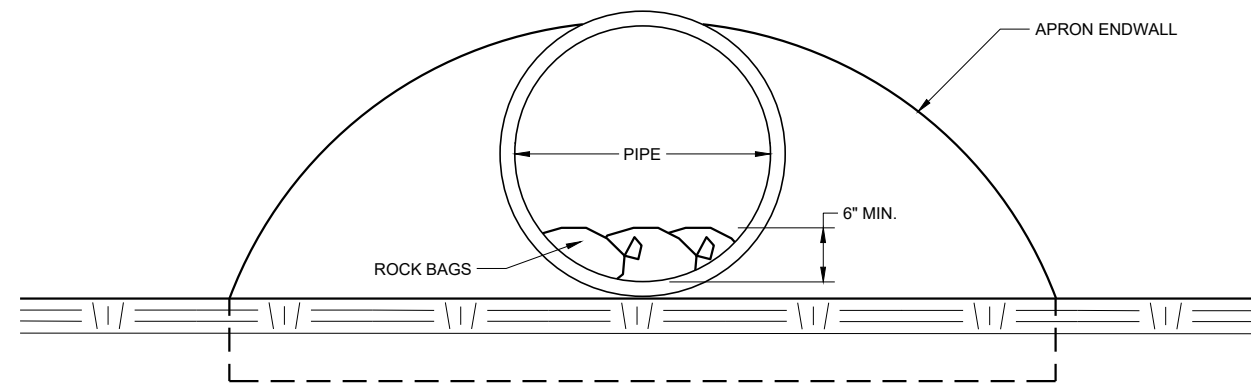
INLET PROTECTION, TYPE D

(CAN BE INSTALLED IN ANY INLET TYPE WITH OR WITHOUT A CURB BOX AS PER NOTE ②)

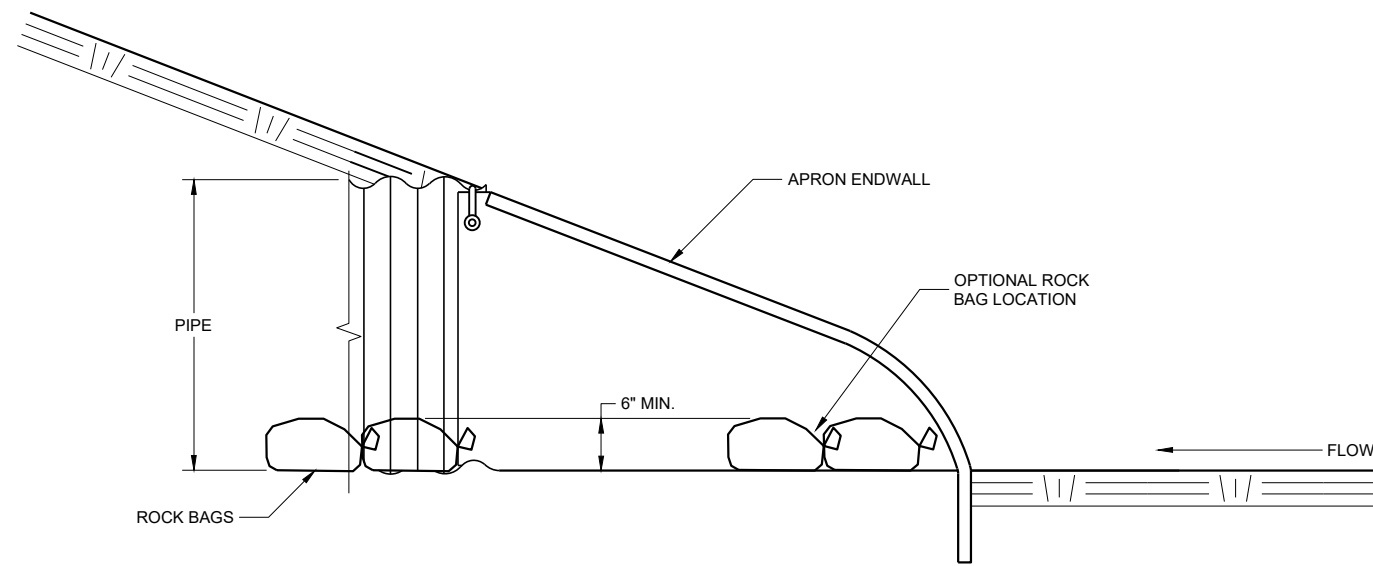
**INLET PROTECTION
TYPE A, B, C, AND D**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
10/16/02 /S/ Beth Connestra
DATE
CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA



END VIEW



SIDE VIEW

CULVERT PIPE CHECK
(INSTALL ON INLET END ONLY)

CULVERT PIPE CHECK

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2019 /S/ Daniel Schave
DATE EROSION CONTROL ENGINEER

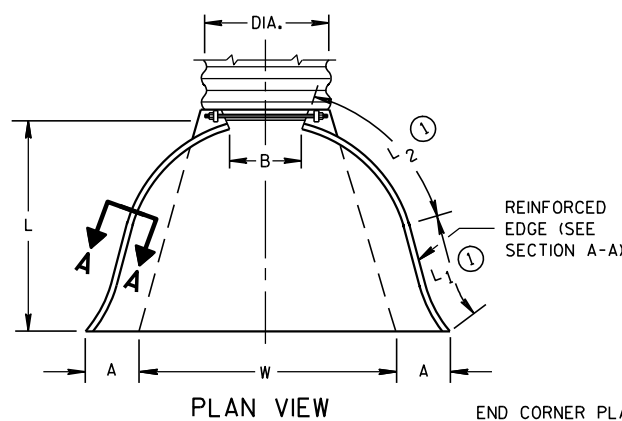
FHWA

METAL APRON ENDWALLS											
PIPE DIA. (IN.)	MIN. THICK. (Inches)		DIMENSIONS (Inches)							APPROX. SLOPE	BODY
	STEEL	ALUM.	A (±1")	B (MAX.)	H (±1")	L (±1 1/2")	L1	L2	W (±2")		
12	.064	.060	6	6	6	21	12	17 1/2	24	2 1/2 to 1	1 Pc.
15	.064	.060	7	8	6	26	14	21 3/4	30	2 1/2 to 1	1 Pc.
18	.064	.060	8	10	6	31	15	28 1/4	36	2 1/2 to 1	1 Pc.
21	.064	.060	9	12	6	36	18	29 5/8	42	2 1/2 to 1	1 Pc.
24	.064	.075	10	13	6	41	18	37 1/4	48	2 1/2 to 1	1 Pc.
30	.079	.075	12	16	8	51	18	52 1/4	60	2 1/2 to 1	1 Pc.
36	.079	.105	14	19	9	60	24	59 3/4	72	2 1/2 to 1	2 Pc.
42	.109	.105	16	22	11	69	24	75 5/8	84	2 1/2 to 1	2 Pc.
48	.109	.105	18	27	12	78	24	81	90	2 1/4 to 1	3 Pc.
54	.109	.105	18	30	12	84	30	85 1/2	102	2 1/4 to 1	3 Pc.
60	.109x	.105x	18	33	12	87	—	—	114	2 to 1	3 Pc.
66	.109x	.105x	18	36	12	87	—	—	120	2 to 1	3 Pc.
72	.109x	.105x	18	39	12	87	—	—	126	2 to 1	3 Pc.
78	.109x	.105x	18	42	12	87	—	—	132	1 1/2 to 1	3 Pc.
84	.109x	.105x	18	45	12	87	—	—	138	1 1/2 to 1	3 Pc.
90	.109x	.105x	18	37	12	87	—	—	144	1 1/2 to 1	3 Pc.
96	.109x	.105x	18	35	12	87	—	—	150	1 1/2 to 1	3 Pc.

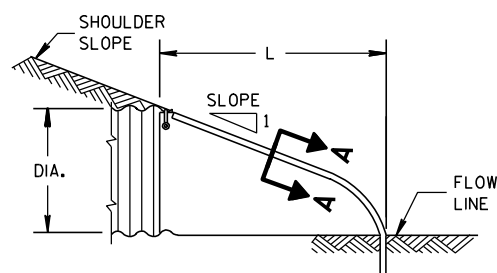
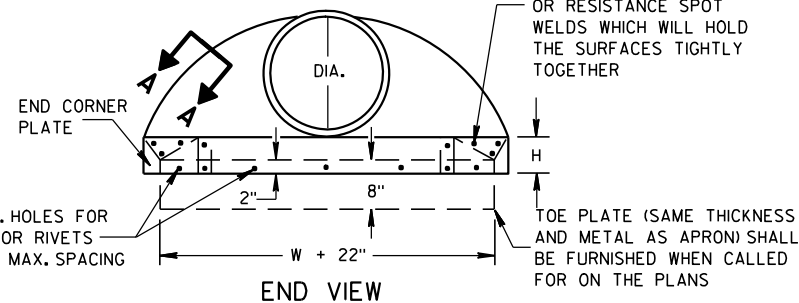
* EXCEPT CENTER PANEL SEE GENERAL NOTES

REINFORCED CONCRETE APRON ENDWALLS									
PIPE DIA. (IN.)	DIMENSIONS (Inches)							APPROX. SLOPE	
	T	A	B	C	D	E	G		
12	2	4	24	48 1/8	72 1/8	24	2	3 to 1	
15	2 1/4	6	27	46	73	30	2 1/4	3 to 1	
18	2 1/2	9	27	46	73	36	2 1/2	3 to 1	
21	2 3/4	9	36	37 1/2	73 1/2	42	2 3/4	3 to 1	
24	3	9 1/2	43 1/2	30	73 1/2	48	3	3 to 1	
27	3 1/4	10 1/2	49 1/2	24	73 1/2	54	3 1/4	3 to 1	
30	3 1/2	12	54	19 3/4	73 1/2	60	3 1/2	3 to 1	
36	4	15	63	34 3/4	97 3/4	72	4	3 to 1	
42	4 1/2	21	63	35	98	78	4 1/2	3 to 1	
48	5	24	72	26	98	84	5	3 to 1	
54	5 1/2	27	65	33 1/4-35	98 1/4-100	90	5 1/2	2 1/2 to 1	
60	6	30-35	60	39	99	96	5	2 to 1	
66	6 1/2	24-30	72-78	21-27	99	102	5 1/2	2 to 1	
72	7	24-36	78	21	99	108	6	2 to 1	
78	7 1/2	24-36	78	21	99	114	6 1/2	2 to 1	
84	8	36	90 1/2	21	111 1/2	120	6 1/2	1 1/2 to 1	
90	8 1/2	41	87 1/2	24	111 1/2	132	6 1/2	1 1/2 to 1	

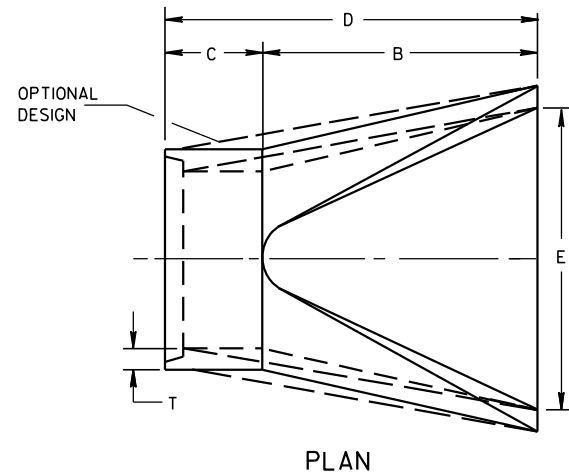
* MINIMUM
** MAXIMUM



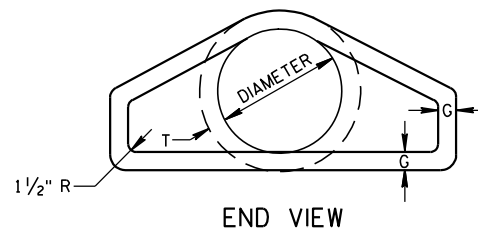
END CORNER PLATES MAY BE FASTENED TO APRON PROPER BY BOLTS, RIVETS, OR RESISTANCE SPOT WELDS WHICH WILL HOLD THE SURFACES TIGHTLY TOGETHER



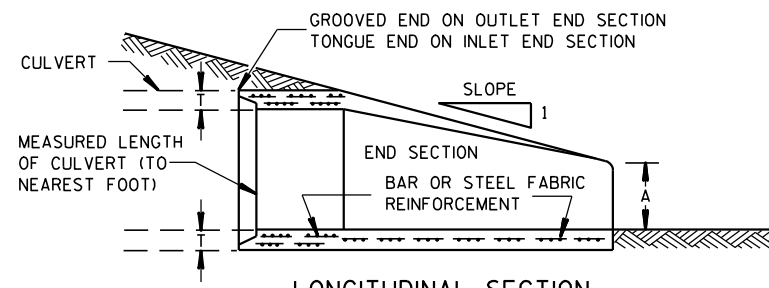
SIDE ELEVATION
METAL ENDWALLS



PLAN

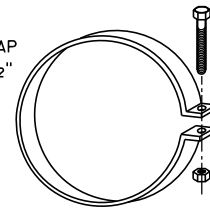


END VIEW

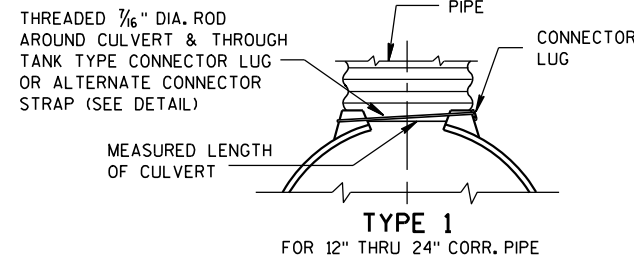


LONGITUDINAL SECTION
CONCRETE ENDWALLS

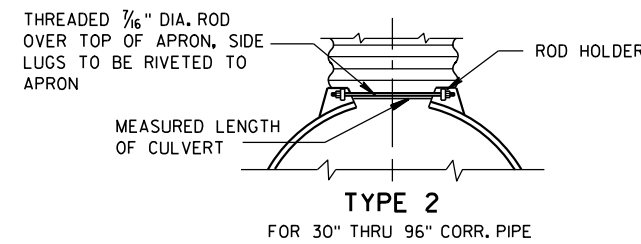
1" WIDE, 12 GA. (0.109" THICK) GALVANIZED STRAP WITH STANDARD 6" X 1/2" BAND BOLT AND NUT



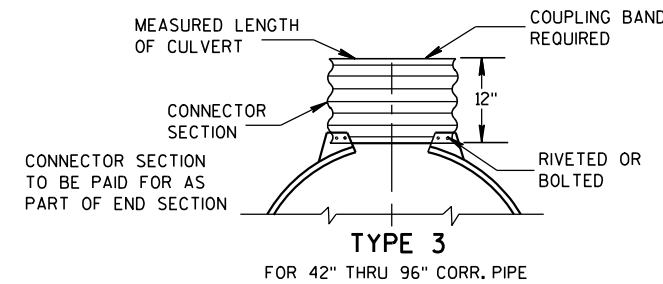
ALTERNATE FOR TYPE 1 CONNECTION
END SECTION CONNECTOR STRAP



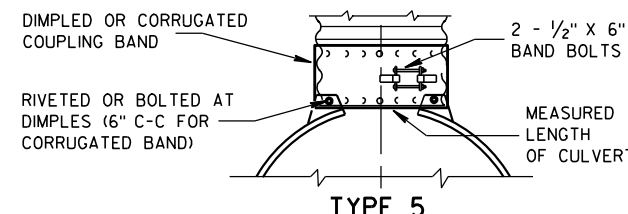
TYPE 1
FOR 12" THRU 24" CORR. PIPE



TYPE 2
FOR 30" THRU 96" CORR. PIPE



TYPE 3
FOR 42" THRU 96" CORR. PIPE



TYPE 5
ALTERNATE FOR:
ALL SIZES CORRUGATED CIRCULAR PIPE

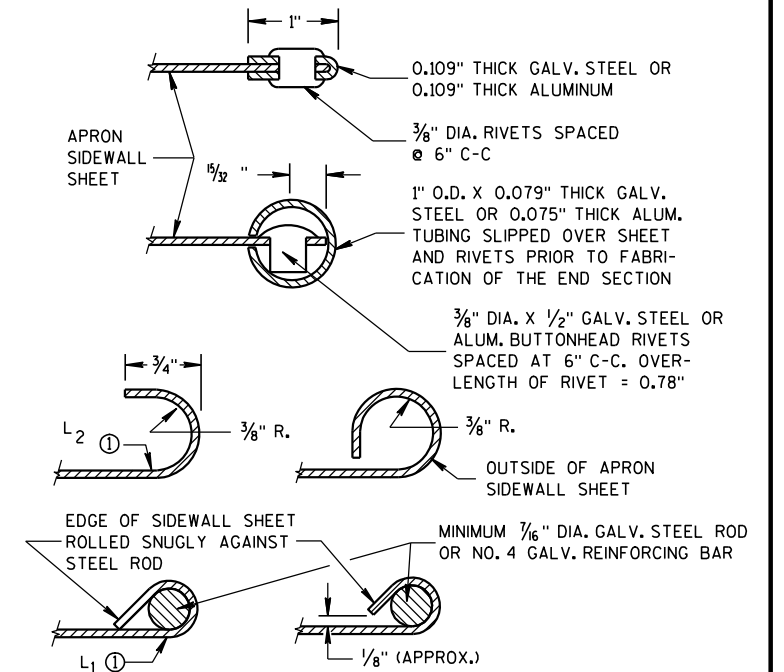
NOTE: DIMPLED BAND FITS OVER OUTSIDE OF ENDWALL, AND CORRUGATED BAND FITS INSIDE ENDWALL. DIMPLED BAND MAY BE USED WITH HELICALLY CORRUGATED PIPE.

FOR CIRCUMFERENTIALLY CORRUGATED PIPE USE ENDWALL CONNECTION DETAILS 1, 2, 3 OR 5 AS APPLICABLE.

FOR HELICALLY CORRUGATED PIPE USE ENDWALL CONNECTION DETAILS 1, 2 OR 5.

FOR HELICALLY CORRUGATED PIPES WITH TWO CIRCUMFERENTIAL CORRUGATIONS AT EACH END USE ENDWALL CONNECTION DETAILS 1, 2 OR 3.

CONNECTION DETAILS



SECTION A-A

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

CONCRETE CULVERT ENDWALLS MAY NOT BE USED WITH GALVANIZED STEEL OR ALUMINUM CULVERT PIPE OR VICE VERSA. GALVANIZED STEEL OR ALUMINUM ENDWALLS SHALL NORMALLY BE INSTALLED ON CULVERT PIPE OF THE SAME METAL.

ALL THREE PIECE STEEL APRON ENDWALLS FOR 60" DIAMETER PIPE AND LARGER SHALL HAVE 0.109" SIDES AND 0.138" CENTER PANELS. ALL THREE PIECE ALUMINUM APRON ENDWALLS FOR 60" DIAMETER PIPE AND LARGER SHALL HAVE 0.105" SIDES AND 0.134" CENTER PANELS. THE WIDTH OF CENTER PANELS SHALL BE GREATER THAN 20 PERCENT OF THE PIPE PERIMETER.

LAP SEAMS SHALL BE TIGHTLY JOINED BY GALVANIZED RIVETS OR BOLTS FOR STEEL UNITS AND ALUMINUM RIVETS AND BOLTS FOR ALUMINUM UNITS. FOR THE 60" THROUGH 96" DIAMETER APRON ENDWALL SIZES, THE REINFORCED EDGES AND CENTER PANEL SEAMS SHALL BE FURTHER REINFORCED WITH GALVANIZED STEEL OR ALUMINUM STIFFENER ANGLES. THE ANGLES SHALL BE ATTACHED BY GALVANIZED NUTS AND BOLTS FOR STEEL UNITS AND ALUMINUM NUTS AND BOLTS FOR ALUMINUM UNITS.

WHERE TWO OR MORE PIPES WITH APRON ENDWALLS ARE LAID ADJACENT TO EACH OTHER, THEY SHALL BE SEPARATED BY A DISTANCE SUFFICIENT TO PROVIDE A MINIMUM CLEARANCE OF 6 INCHES BETWEEN APRON ENDWALLS.

① FOR PIPE SIZES UP TO 60" DIAMETER, A 180° ROLLED EDGE MAY BE USED INSTEAD OF STEEL ROD REINFORCEMENT. SEE SECTION A-A.

APRON ENDWALLS FOR
CULVERT PIPE

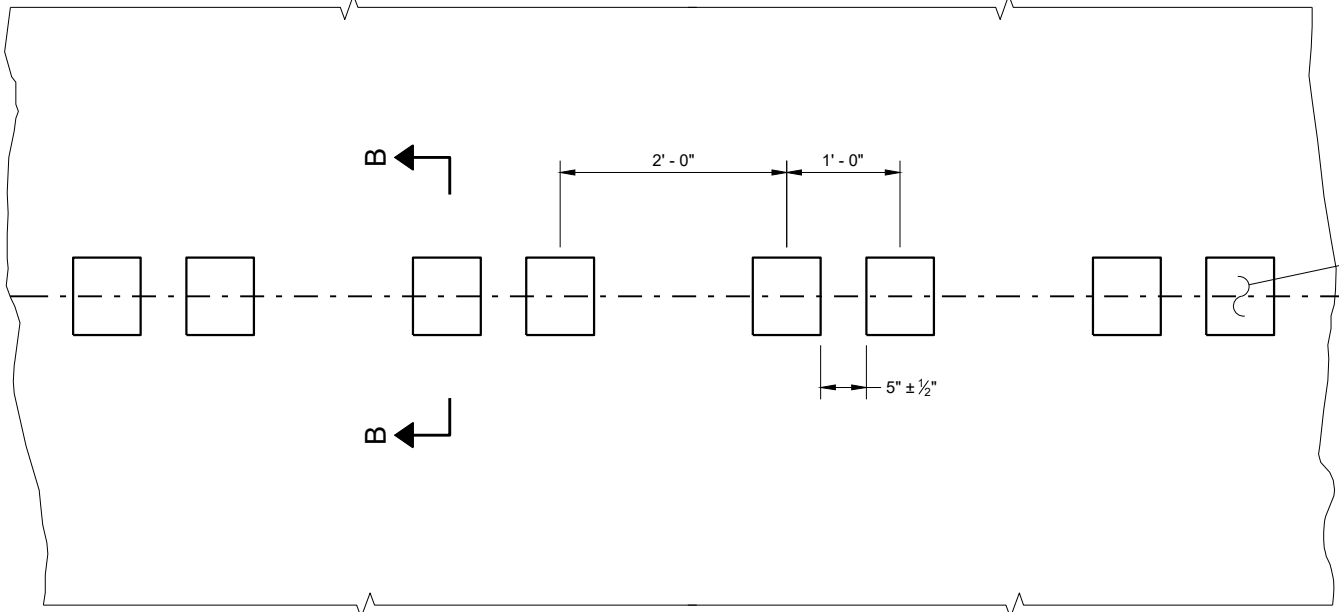
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
11/30/94 DATE /S/ Rory L. Rhinesmith
DATE CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA

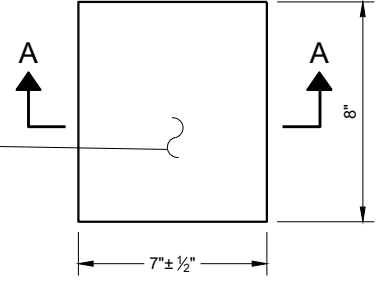
GENERAL NOTES

DO NOT MILL SHOULDER GROOVES THROUGH INTERSECTIONS, MARKED CROSSWALKS, NON-MOTORIZED PATH CROSSINGS, ETC. REFER TO SDD 13A11 SHEETS "d" AND "e".

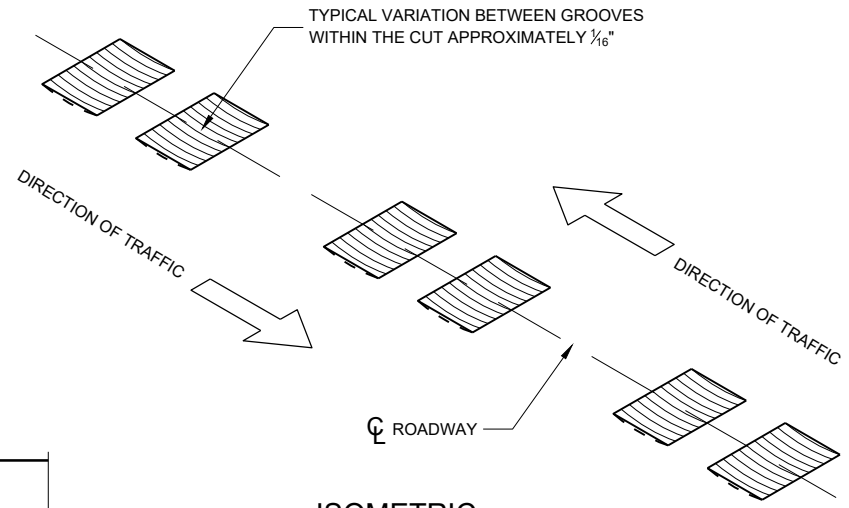
CENTERLINE GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS WHEN DIRECTED BY THE ENGINEER.



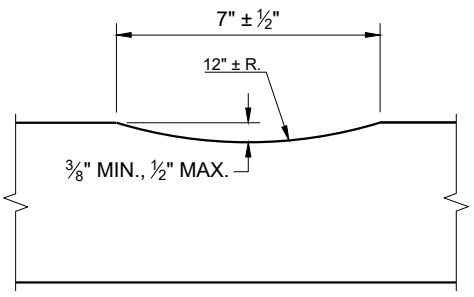
PLAN DETAIL VIEW



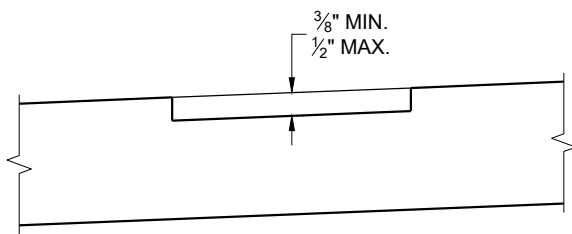
PLAN VIEW (SINGLE GROOVE)



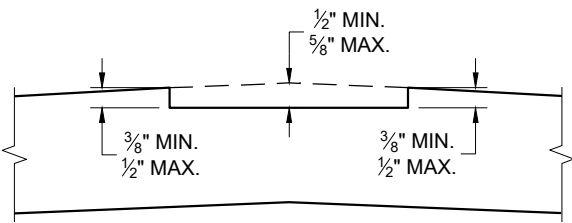
ISOMETRIC



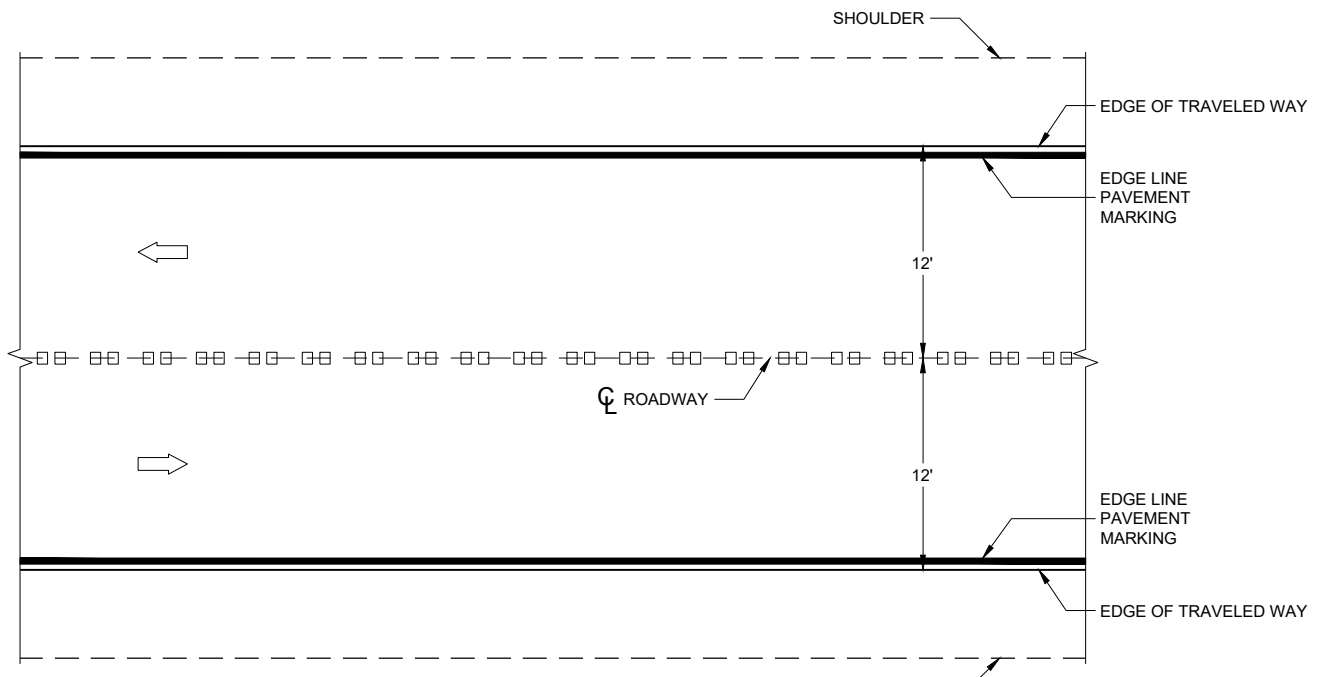
SECTION A - A



SECTION B - B SUPERELEVATED ROADWAY



SECTION B - B CROWNED ROADWAY



PLAN VIEW

CENTERLINE RUMBLE STRIPS - ASPHALT

CENTERLINE RUMBLE STRIPS - ASPHALT
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

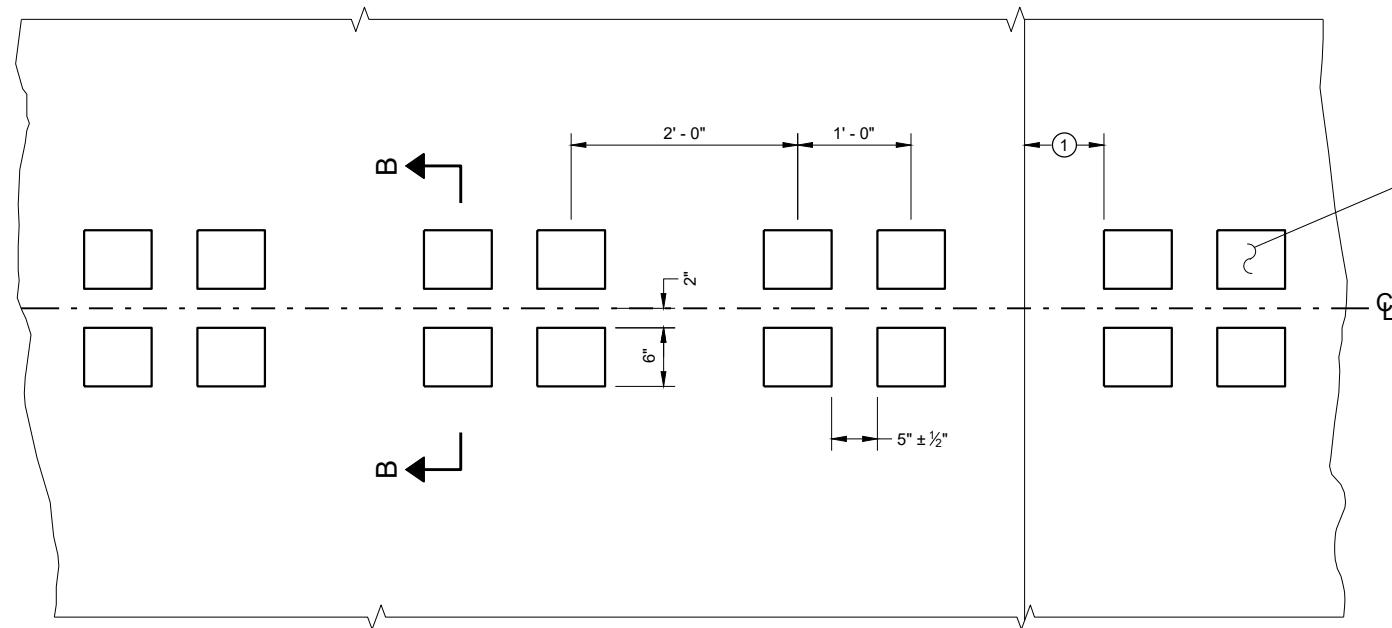
GENERAL NOTES

SDD 13A11, SHEET "d" SHOWS THE LOCATION OF RUMBLE STRIPS AT INTERSECTIONS, INTERSECTIONS WITH LEFT TURN LANES, BRIDGES, COMMERCIAL AND RESIDENTIAL DRIVEWAYS AND RAILROAD CROSSINGS.

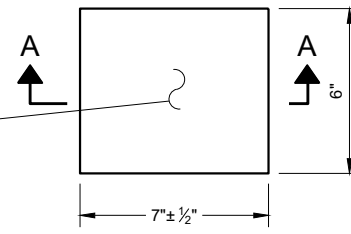
TEMPORARY PAVEMENT MARKINGS ARE TYPICALLY PLACED PRIOR TO RUMBLE STRIP INSTALLATION. PERMANENT MARKINGS ARE INSTALLED AFTER RUMBLE STRIP INSTALLATION.

CENTERLINE GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS WHEN DIRECTED BY THE ENGINEER.

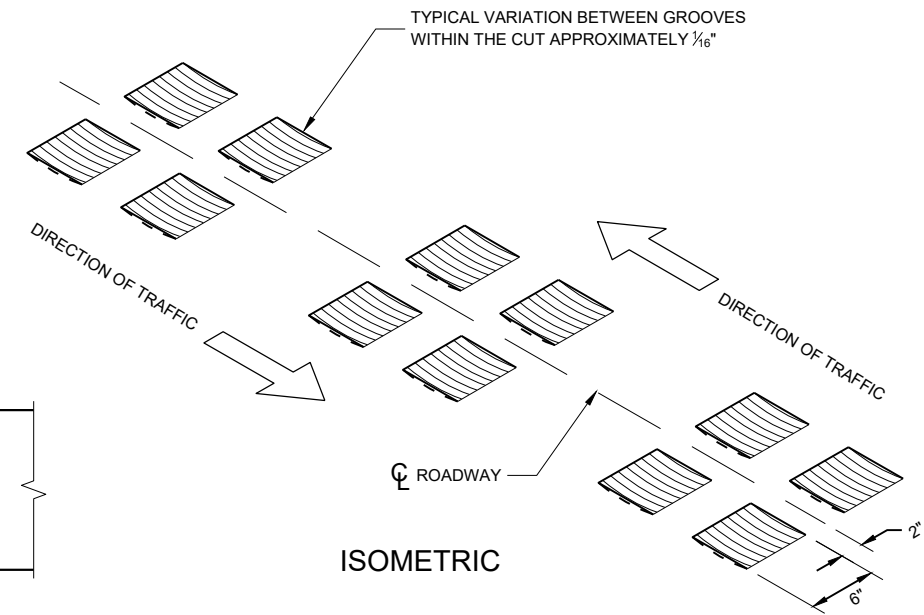
① CONCRETE PAVEMENT RUMBLE STRIPS SHALL BE A MINIMUM OF 6 INCHES FROM TRANSVERSE JOINTS.



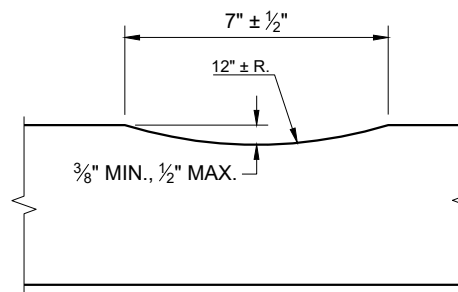
PLAN DETAIL VIEW



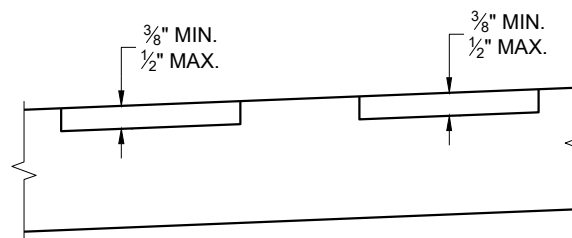
**PLAN VIEW
(SINGLE GROOVE)**



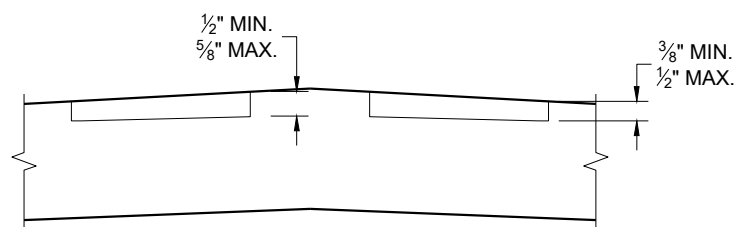
ISOMETRIC



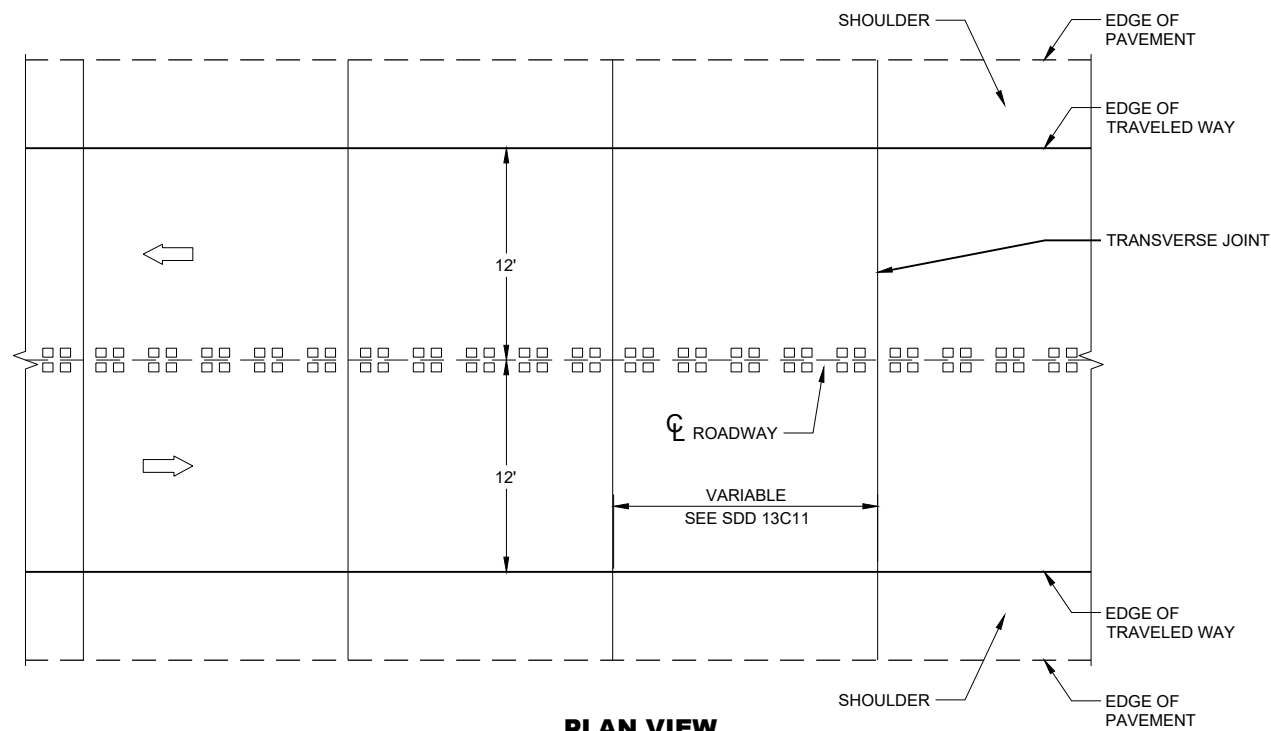
SECTION A - A



**SECTION B - B
SUPERELEVATED ROADWAY**



**SECTION B - B
CROWNED ROADWAY**

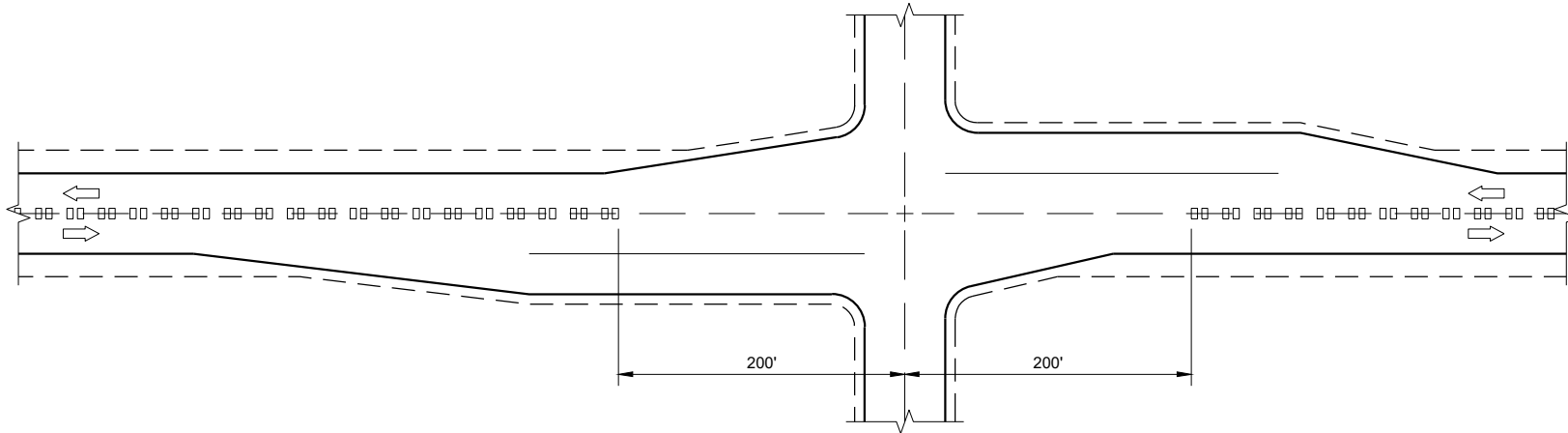


PLAN VIEW

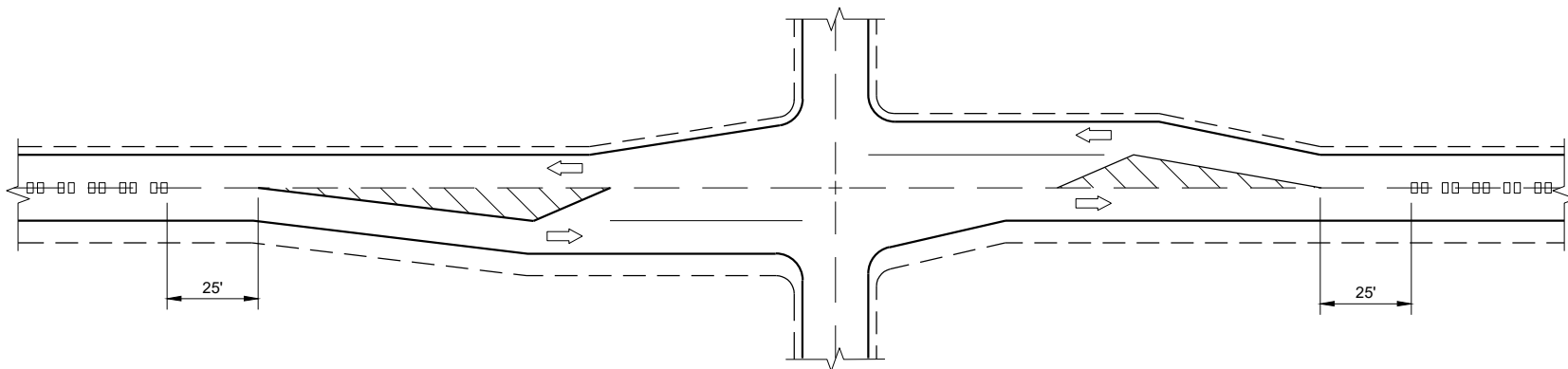
CENTERLINE RUMBLE STRIPS - CONCRETE

CENTERLINE RUMBLE STRIPS - CONCRETE

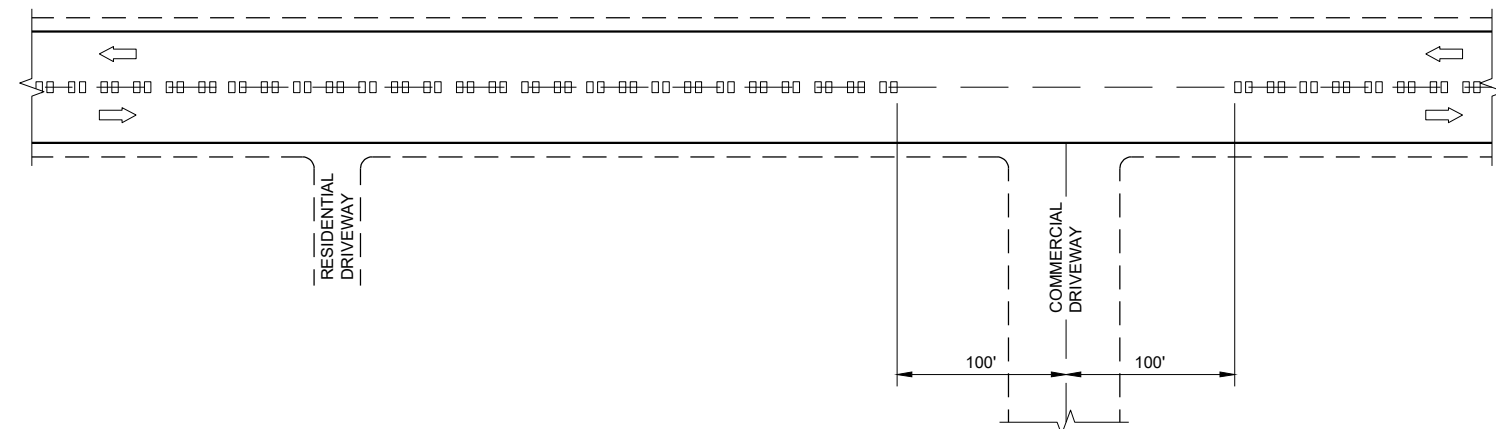
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



CENTERLINE GROOVES AT INTERSECTIONS



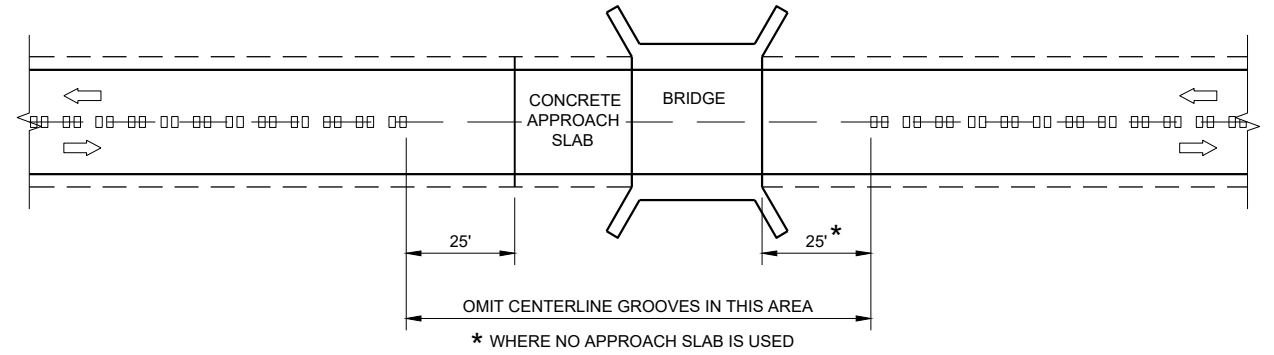
**CENTERLINE GROOVES AT INTERSECTIONS
(WITH LEFT TURN LANES)**



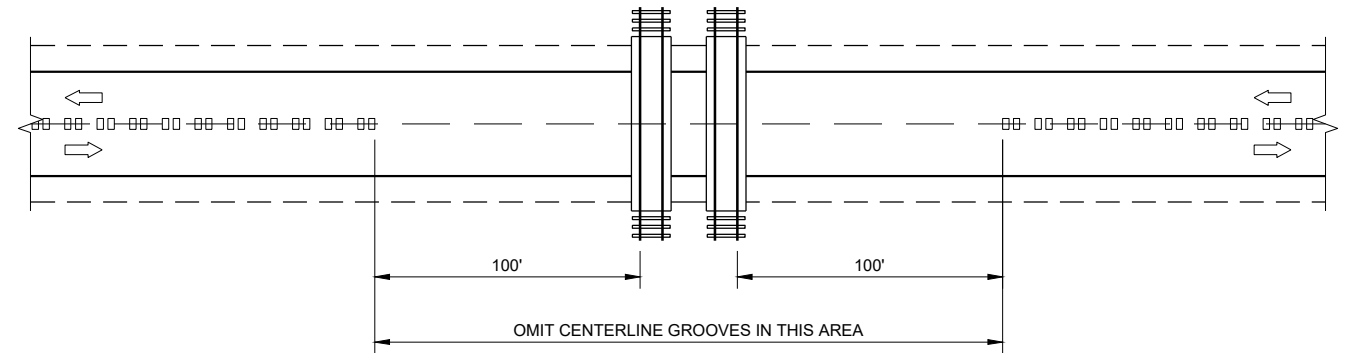
CENTERLINE GROOVES AT DRIVEWAYS^①

GENERAL NOTES

- ① CENTERLINE GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS WHEN DIRECTED BY THE ENGINEER.



CENTERLINE GROOVES AT BRIDGES



CENTERLINE GROOVES AT RAILROADS

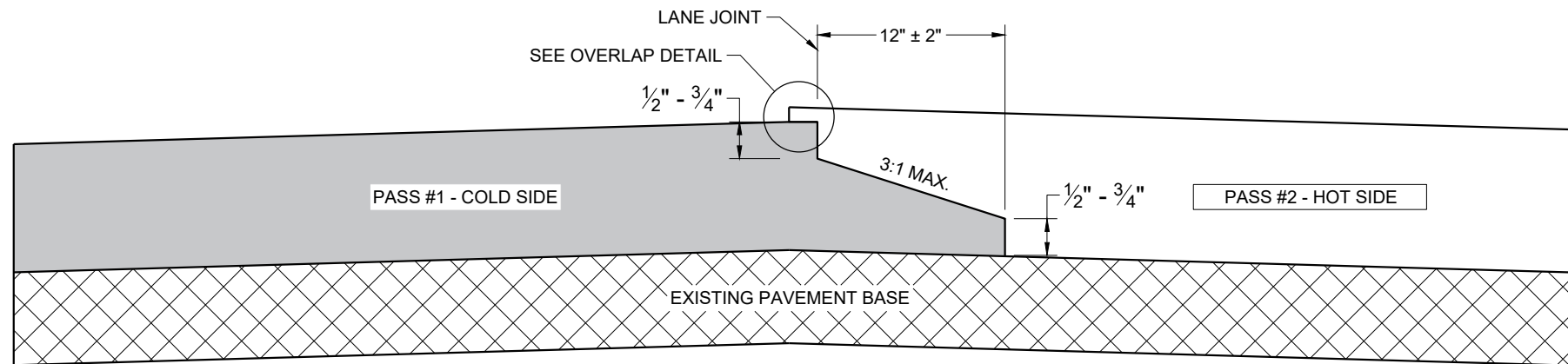
6

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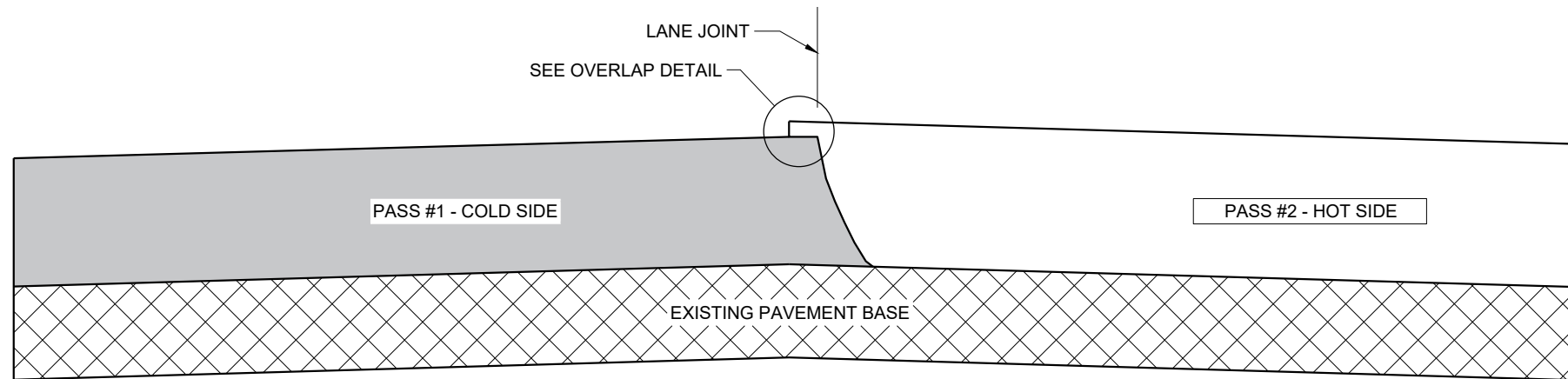
SDD 13A11 - 04d

SDD 13A11 - 04d

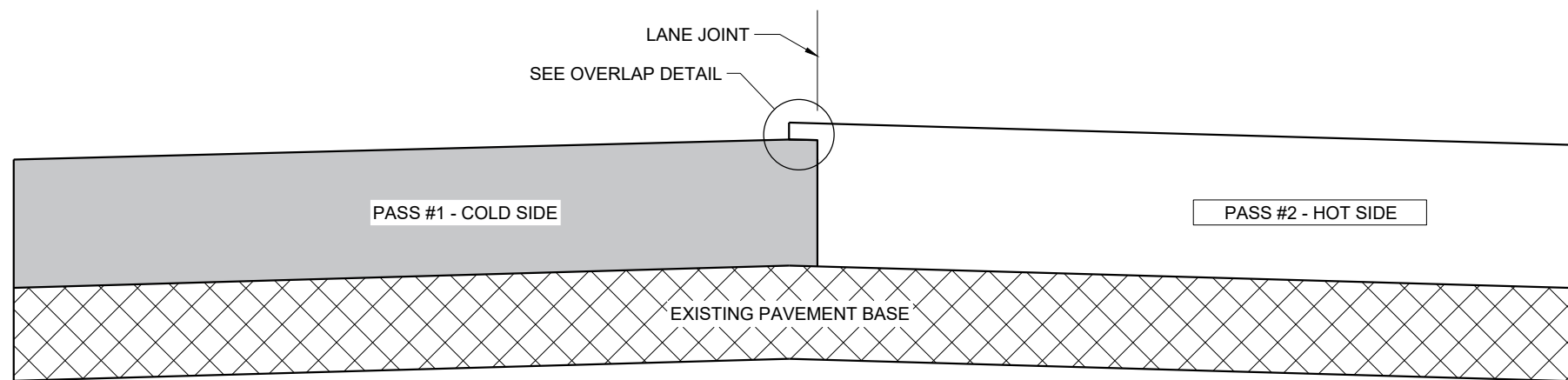
CENTER LINE RUMBLE STRIPS - INTERSECTIONS, DRIVEWAYS, BRIDGES, RAIL ROADS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2023 DATE	/S/ John Jenkins ROADWAY STANDARDS DEVELOPMENT ENGINEER
<small>FHWA</small>	



**TYPICAL PAVEMENT CROSS SECTION
NOTCHED WEDGE JOINT**



**TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT**



**TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT (MILLED)**

GENERAL NOTES

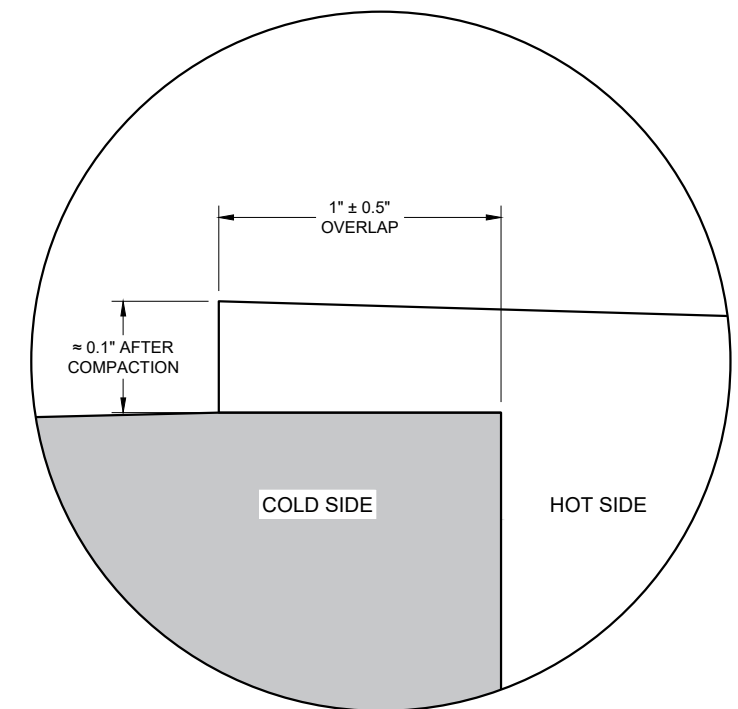
IN ADDITION TO THE DETAILS PROVIDED IN THIS DRAWING, CONFORM TO STANDARD SPECIFICATION 450.3.2.8 FOR WHEN A NOTCHED WEDGE JOINT IS REQUIRED AND FOR GENERAL JOINT CONSTRUCTION REQUIREMENTS.

FOR ALL LONGITUDINAL JOINTS, ENSURE THE PAVER SCREED OVERLAPS THE PREVIOUSLY PLACED PAVEMENT BY $1" \pm 0.5"$ AND THE HOT SIDE OF THE JOINT REMAINS HIGHER THAN THE COLD SIDE BY APPROXIMATELY $0.1"$ AFTER FINAL COMPACTION. (IT WILL BE FLUSH WHEN PAVING IN ECHELON.)

ONLY REMOVE THE LONGITUDINAL NOTCHED WEDGE JOINT FOR SMA PAVEMENT OR AS DIRECTED BY THE ENGINEER TO ADDRESS SPECIFIC LENGTHS OF JOINT DAMAGED BY TRAFFIC.

WHEN MILLING BACK OR REMOVING ANY LONGITUDINAL JOINT, LIMIT THE MATERIAL REMOVED TO $2"$ FROM THE TOP NOTCH OR FROM THE VERTICAL JOINT EDGE ON THE COLD SIDE OF THE JOINT.

USE LONGITUDINAL MILLED JOINT AS PLANS SHOW OR THE AS THE ENGINEER DIRECTS.



OVERLAP DETAIL (TYPICAL)

6

6

SDD 13C19 - 03

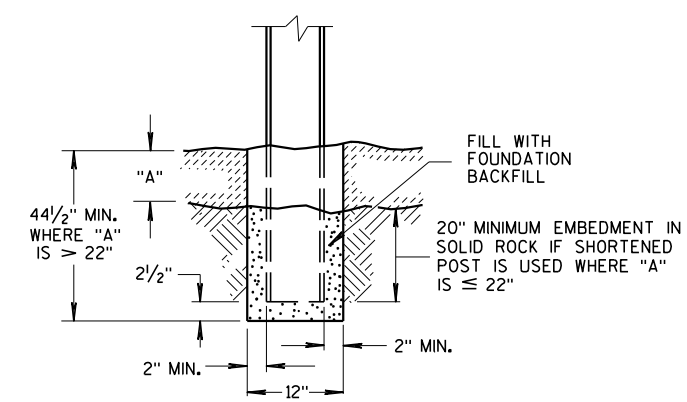
SDD 13C19 - 03

HMA LONGITUDINAL JOINTS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED November 2020 DATE	/S/ Steven Hefel HMA PAVEMENT ENGINEER
FHWA	

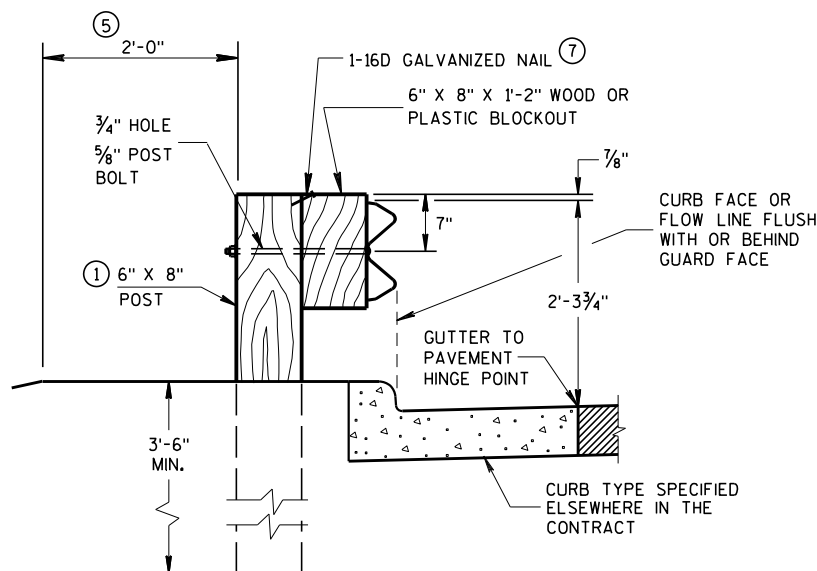
GENERAL NOTES

- ① W6 X 9 OR W6 X 8.5 STEEL POSTS AND NOTCHED PLASTIC BLOCKOUTS ARE ACCEPTABLE ALTERNATIVES FOR 6" X 8" WOOD POSTS WITH WOOD OR PLASTIC BLOCKOUTS. USE APPROVED NOTCHED PLASTIC BLOCKOUTS WITH STEEL POSTS. APPROVED PLASTIC BLOCKOUT DESIGNS MAY VARY FROM THIS TYPICAL DETAIL WHEN USED IN CONJUNCTION WITH STEEL POSTS. DO NOT MIX STEEL POSTS AND WOOD POSTS IN A SINGLE INSTALLATION.
- ② USE STRUCTURAL STEEL POSTS CONFORMING TO ASTM A 36. GALVANIZED POSTS ACCORDING TO AASHTO M 111. EITHER SET THE POSTS IN DRILLED HOLES OR DRIVE TO GRADE. REMOVE MUSHROOMING CAUSED BY DRIVING AND REPAIR DAMAGED SPELTER COATING ON GALVANIZED POSTS.
- ③ INSTALL STEEL POSTS WITH HOLES ON APPROACHING TRAFFIC SIDE.
- ④ USE EITHER WOOD OR APPROVED PLASTIC BLOCKOUTS ON WOOD POSTS.
- ⑤ IF THE DISTANCE FROM BACK OF POST TO SHOULDER HINGE POINT IS LESS THAN 2 FEET INSTALL LONGER POST AT HALF POST SPACING, W BEAM (LHW).
- ⑥ IF ROCK IS ENCOUNTERED DURING EXCAVATION, THE ENGINEER MAY APPROVE USING A 12 INCH DIAMETER POST HOLE EXTENDING 20 INCHES DEEP INTO THE ROCK. PLACE GRANULAR MATERIAL IN THE BOTTOM OF THE HOLE APPROXIMATELY 2 1/2 INCHES DEEP. CUT THE POSTS TO LENGTH AND PLACE IN THE HOLE. BACKFILL WITH MATERIAL EXCAVATED FROM THE HOLE AND COMPACT ADEQUATELY.
- ⑦ WHEN USING STEEL POSTS AND WOOD BLOCKOUTS INSTALL FOUR 16D GALVANIZED NAILS. INSTALL NAILS AT THE BACK CORNERS OF THE BLOCK AND BEND THE NAILS OVER THE FLANGE OF THE STEEL POST.

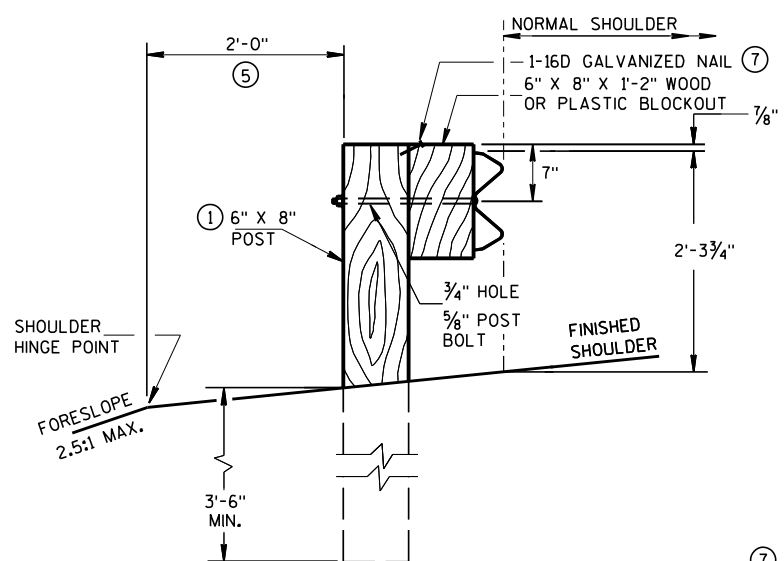
INSTALL BEAM GUARD SECTIONS AND ALL NECESSARY HARDWARE ACCORDING TO THE APPLICABLE PLAN AND CURRENT STANDARD AND SUPPLEMENTAL SPECIFICATIONS. ALL DIMENSIONS ARE SUBJECT TO MANUFACTURER'S TOLERANCES EXCEPT WHERE ALLOWABLE TOLERANCES ARE SHOWN.



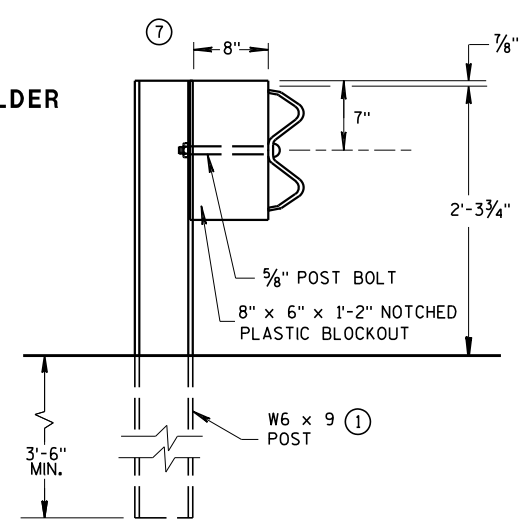
END VIEW SETTING STEEL OR WOOD POST IN ROCK ⑥



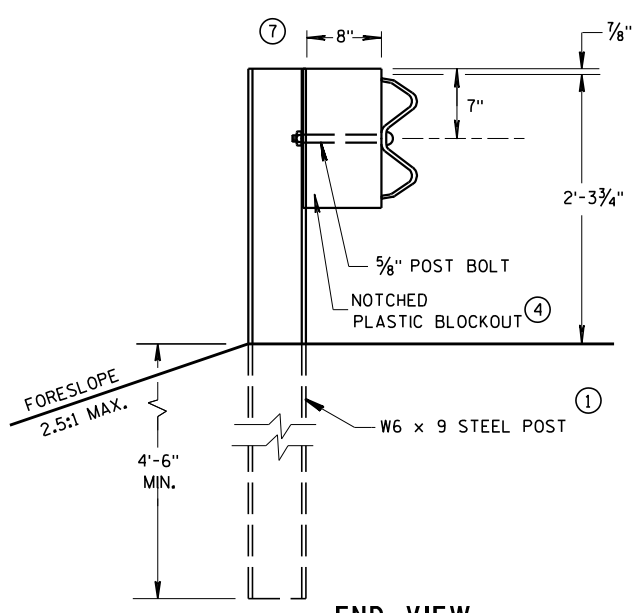
END VIEW LOCATED ALONG A CURBED ROADWAY



END VIEW LOCATED ALONG A ROADWAY SHOULDER STANDARD INSTALLATION

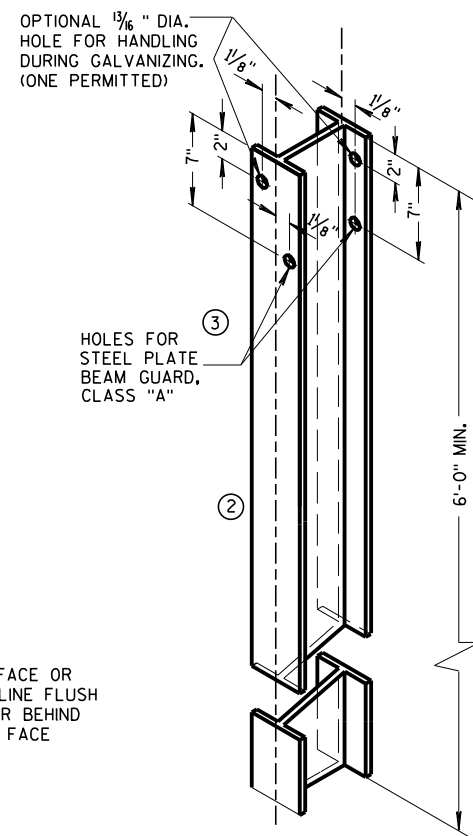


END VIEW STEEL POST & NOTCHED PLASTIC BLOCKOUT ALTERNATIVE STANDARD INSTALLATION

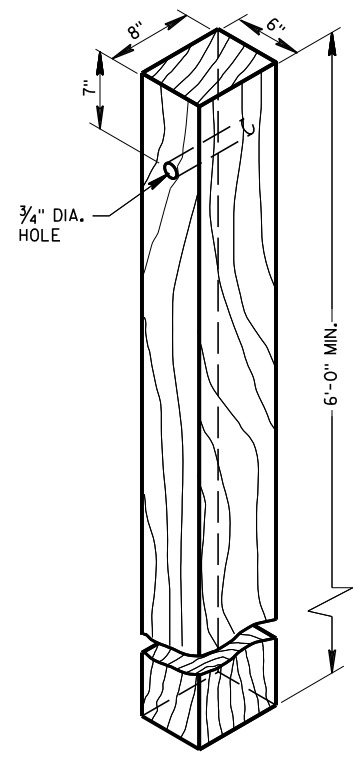


END VIEW LONGER POST AT HALF POST SPACING W BEAM (LHW)

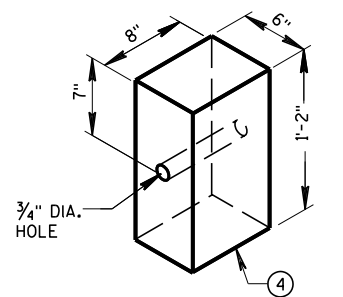
TYPICAL INSTALLATION OF STEEL PLATE BEAM GUARD



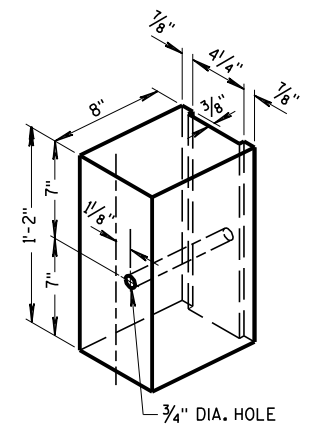
STEEL POST & HOLE PUNCHING DETAIL (W6 X 9) ①
ALL HOLES 3/8" DIAMETER EXCEPT AS NOTED



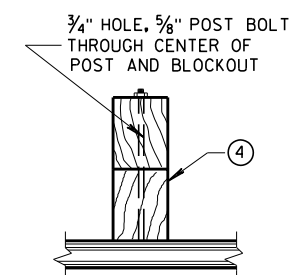
WOOD POST (6" X 8") NOMINAL



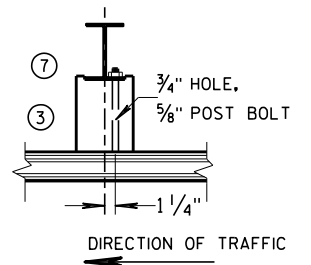
WOOD OR PLASTIC BLOCKOUT FOR WOOD POSTS



TYPICAL NOTCHED PLASTIC BLOCKOUT FOR STEEL POSTS ①



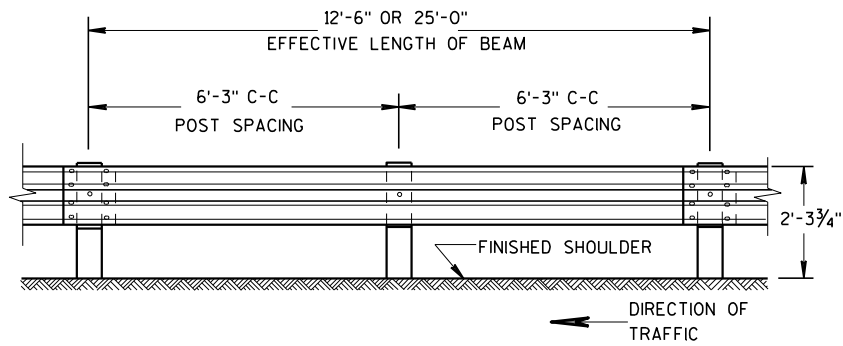
PLAN VIEW WOOD POST, BLOCKOUT & BEAM



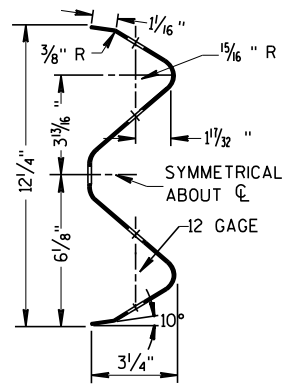
PLAN VIEW STEEL POST, NOTCHED PLASTIC BLOCKOUT & BEAM

STEEL PLATE BEAM GUARD, CLASS "A" INSTALLATION & ELEMENTS

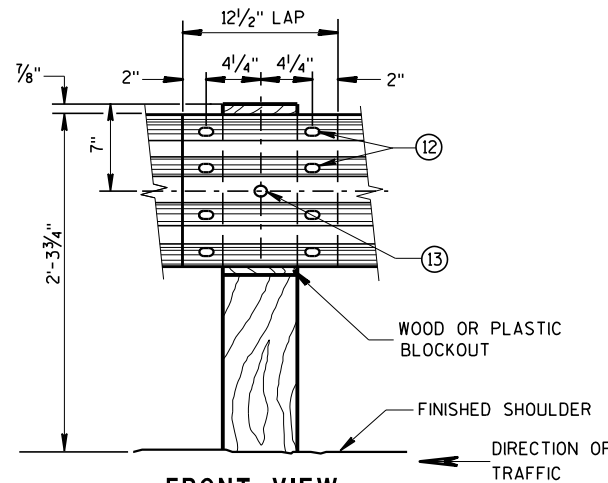
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



**FRONT VIEW
POST SPACING STANDARD INSTALLATION**



SECTION THRU W BEAM

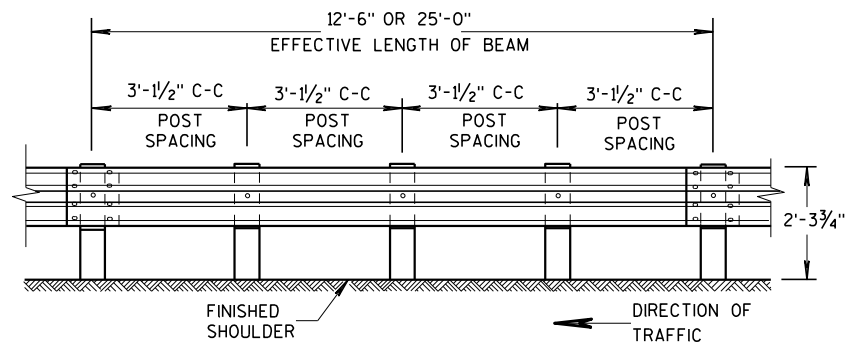


**FRONT VIEW
BEAM SPLICE AT WOOD POST
AND POST MOUNTING DETAIL**

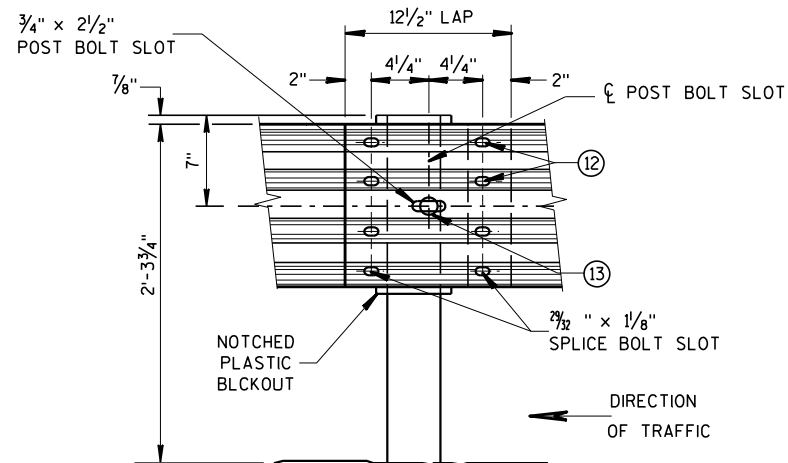
GENERAL NOTES

FURNISH GUARDRAIL DEFLECTORS FROM APPROVED PRODUCTS LIST.

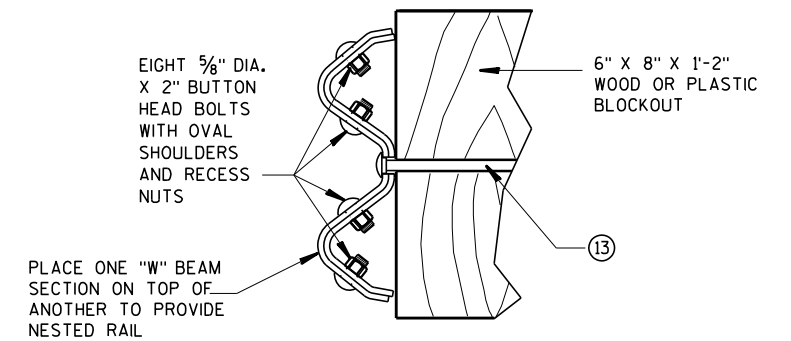
- ⑨ DO NOT INSTALL REFLECTORS ON THE FIRST 50 FEET OF THE APPROACH END OF THE ENERGY ABSORBING TERMINA. START REFLECTORS AT POST #9 AND SPACE EVENLY EVERY 100 FEET (MAX.) TO THE END OF GUARDRAIL RUN, USING A MINIMUM OF 3 REFLECTORS.
- ⑫ 8 - 5/8" ϕ X 2" BUTTON HEAD BOLTS WITH OVAL SHOULDERS & RECESS NUTS.
- ⑬ 5/8" DIA. BUTTON HEAD BOLT AND RECESS NUT WITH 5/8" DIA. F844 FLAT WASHER UNDER NUT.



**FRONT VIEW
POST SPACING FOR LONGER POST
AT HALF POST SPACING W BEAM (LHW)**

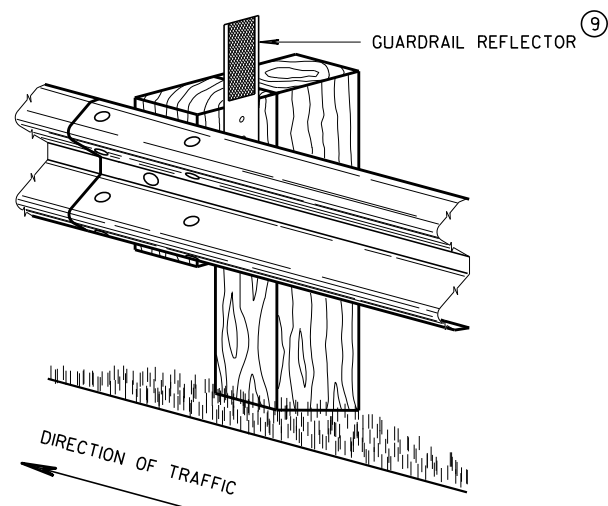


**FRONT VIEW
BEAM SPLICE AT STEEL POST
TYPICAL SPLICING DETAILS
OF STEEL PLATE BEAM GUARD**

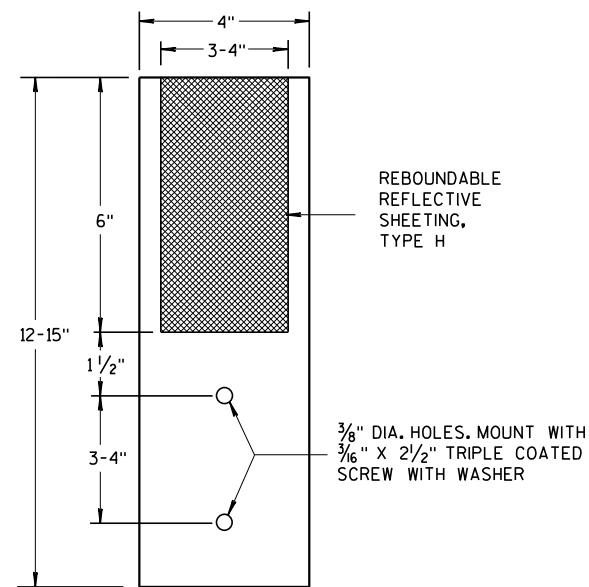


NESTED W BEAM (NW)
USE ALL OTHER STANDARD BEAM GUARD DETAILS FOR
CONSTRUCTING NESTED W BEAM (NW)

* USE DOUBLE SIDED WHITE GUARDRAIL REFLECTORS ON ROADWAYS WITH BI-DIRECTIONAL TRAFFIC (NO MEDIAN). USE SINGLE SIDED WHITE (RIGHT SIDE) AND SINGLE SIDED YELLOW (LEFT SIDE) ON ROADWAYS WITH MEDIAN SEPARATION.



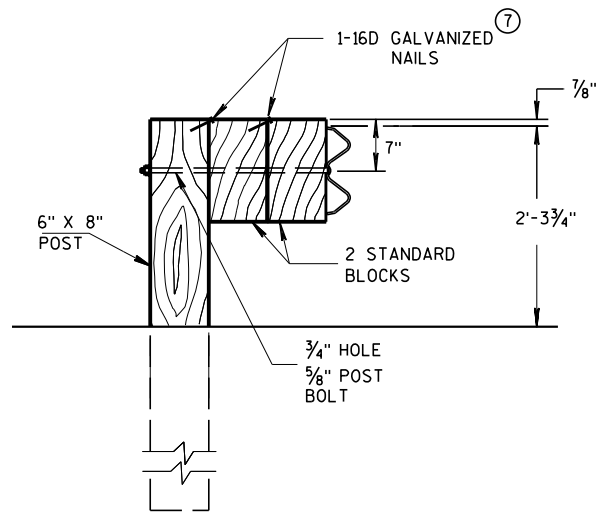
**4" X 12" GUARDRAIL REFLECTOR DETAIL
AND TYPICAL INSTALLATION ***



4" x 12" GUARDRAIL REFLECTOR

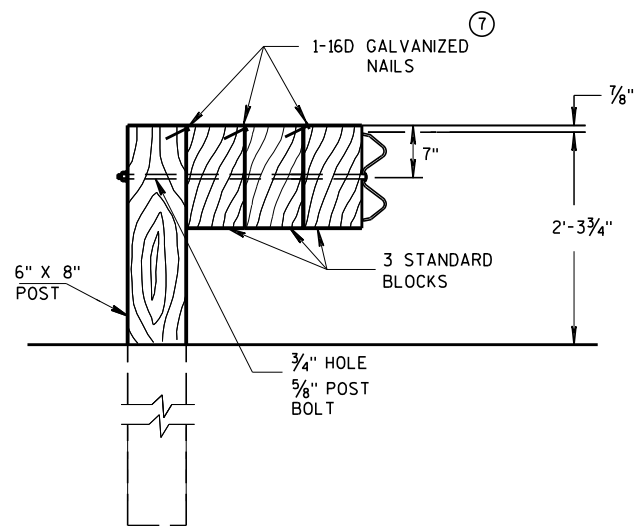
**STEEL PLATE BEAM GUARD,
CLASS "A",
INSTALLATION & ELEMENTS**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



DETAIL FOR DOUBLE BLOCKS

THE NUMBER OF DOUBLE BLOCK POSTS WITHIN A BARRIER RUN IS UNLIMITED

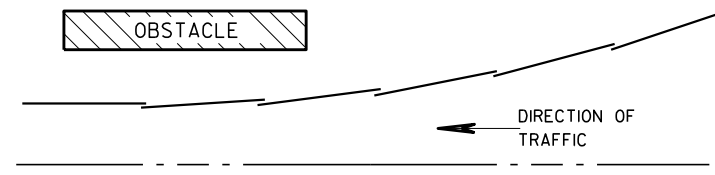


DETAIL FOR TRIPLE BLOCKS

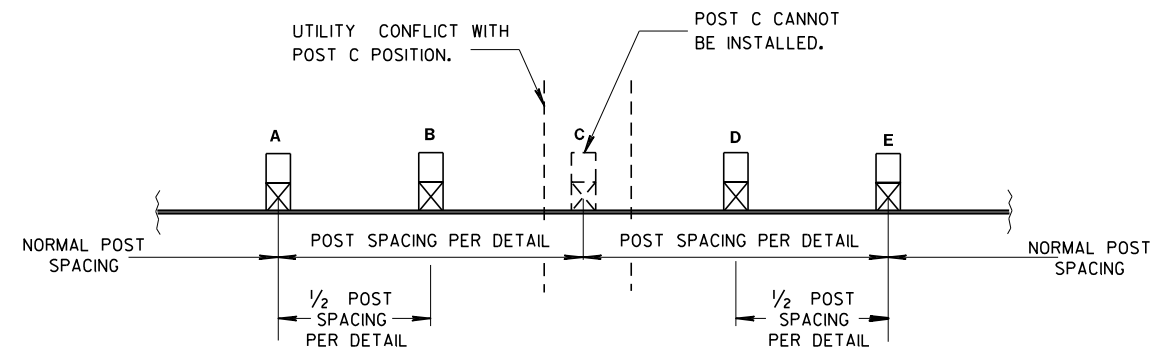
TRIPLE BLOCK DETAIL IS LIMITED TO ONE LOCATION WITHIN A BEAM GUARD RUN.

NOTES: USE DOUBLE OR TRIPLE BLOCKS WHEN UNDERGROUND OBSTACLES PREVENT THE POST FROM BEING INSTALLED.

DO NOT USE EXTRA BLOCKOUTS IF IT CAUSES THE POST TO BE DRIVEN BEYOND SHOULDER HINGE POINT OR CAUSES A FIXED OBJECT TO BE WITHIN THE DEFLECTION DISTANCE OF THE BARRIER.

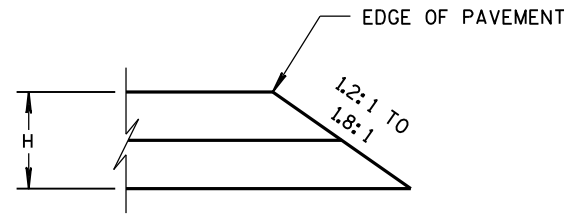


**PLAN VIEW
BEAM LAPPING DETAIL**

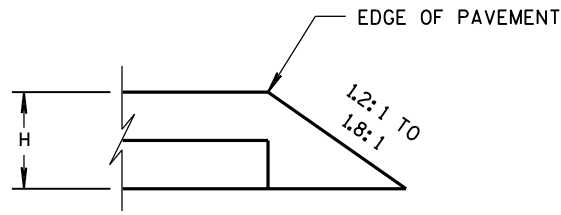


**POST DRIVING FOR CONTINUOUS
UNDERGROUND OBSTRUCTION**

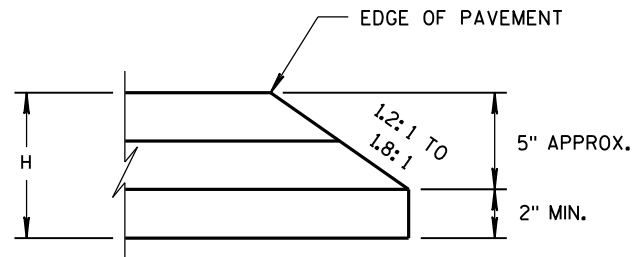
STEEL PLATE BEAM GUARD, CLASS "A", INSTALLATION & ELEMENTS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED June 2017	/s/ Rodney Taylor
DATE	ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR
FHWA	



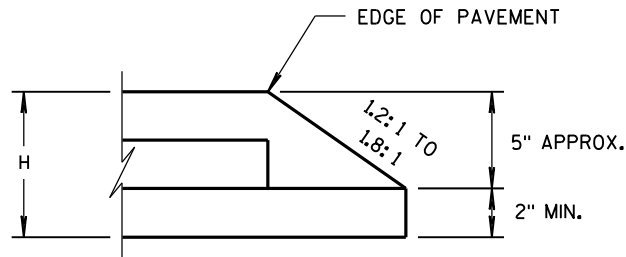
CONSTRUCTED WITH FINAL TWO LAYERS
FOR H 5" OR LESS



CONSTRUCTED WITH FINAL LAYER
FOR H 5" OR LESS

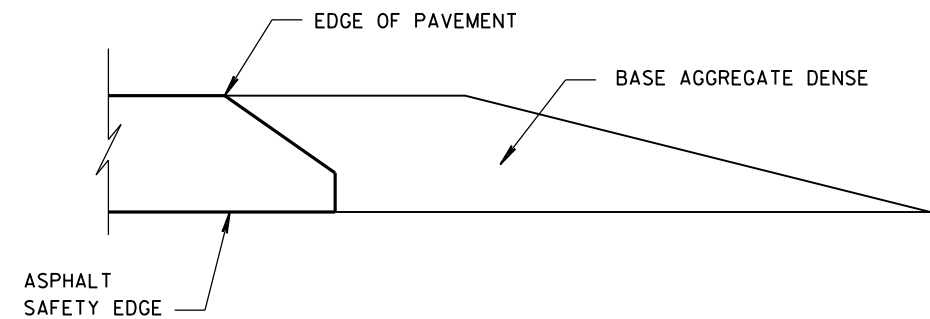


CONSTRUCTED WITH FINAL TWO LAYERS
FOR H GREATER THAN 5"



CONSTRUCTED WITH FINAL LAYER
FOR H GREATER THAN 5"

HMA PAVEMENT AND HMA OVERLAYS



FINISHED SHOULDER AGGREGATE PLACEMENT

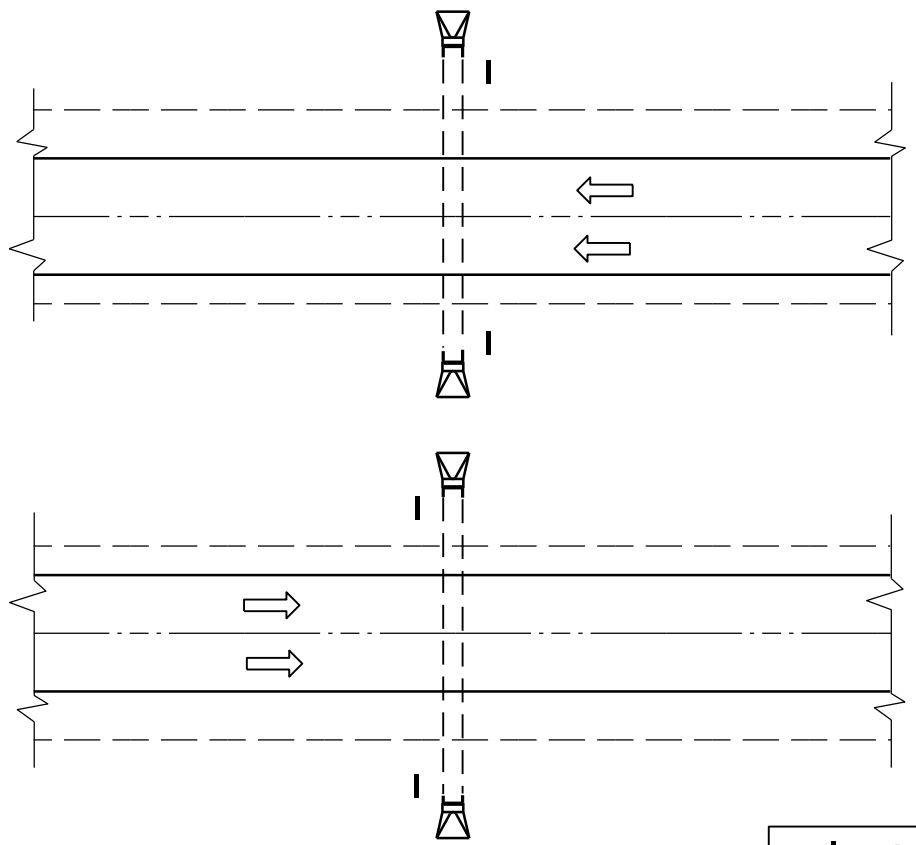
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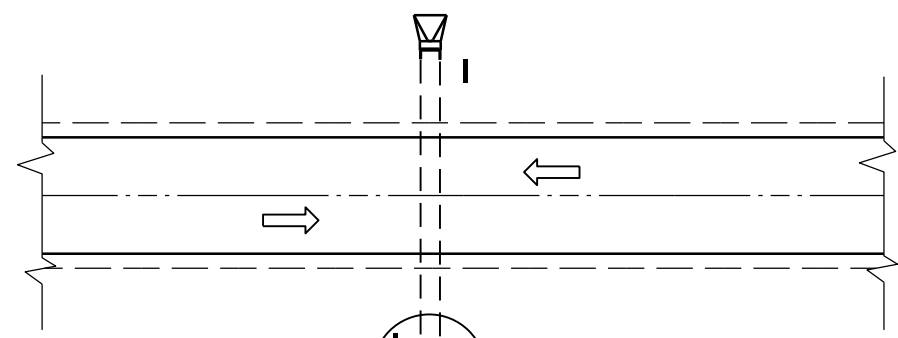
S.D.D. 14 B 29-1

S.D.D. 14 B 29-1

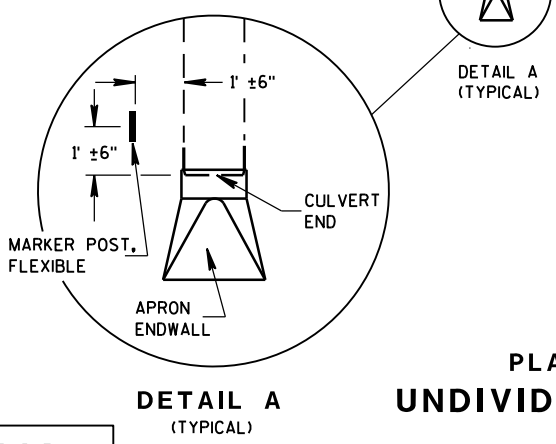
SAFETY EDGE _{SM}	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED DATE	/s/ Jerry H. Zogg ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA	



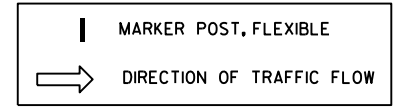
PLAN VIEW
DIVIDED HIGHWAY



PLAN VIEW
UNDIVIDED HIGHWAY

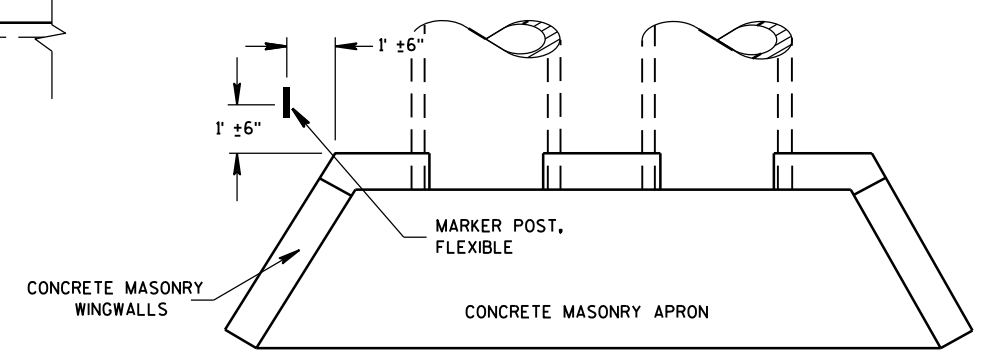


DETAIL A
(TYPICAL)



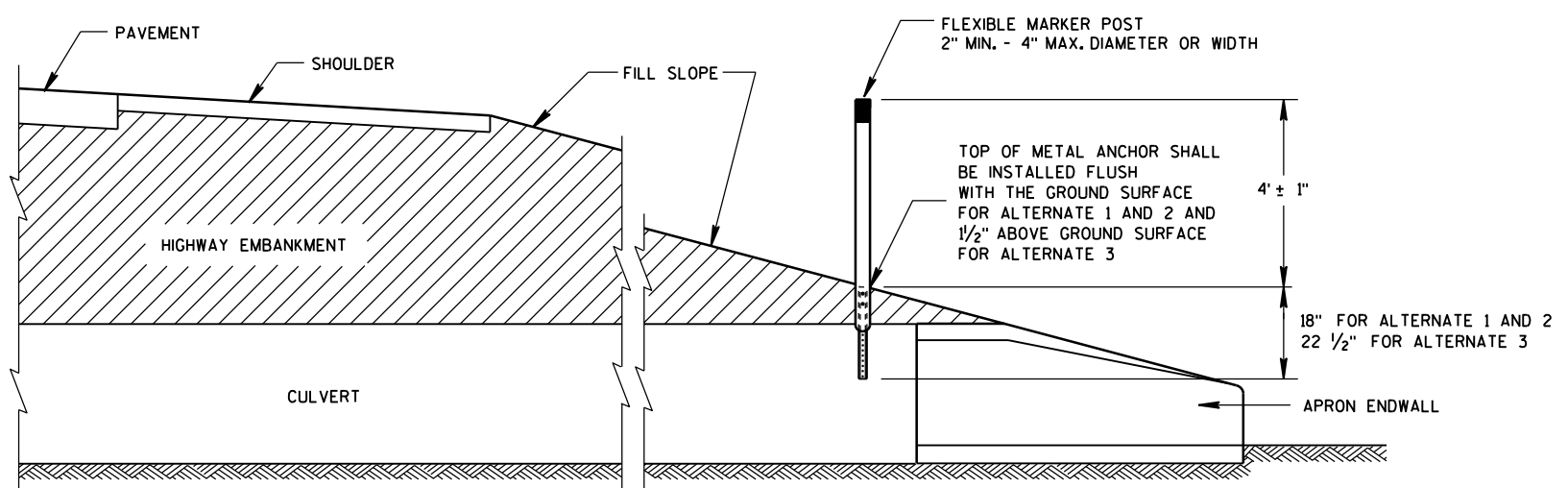
GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.



PLAN VIEW
CONCRETE MASONRY ENDWALLS FOR
CULVERT PIPE AND PIPE ARCH

FLEXIBLE MARKER POST LOCATION



CROSS SECTION
FLEXIBLE MARKER POST

**FLEXIBLE MARKER POST
FOR CULVERT END**

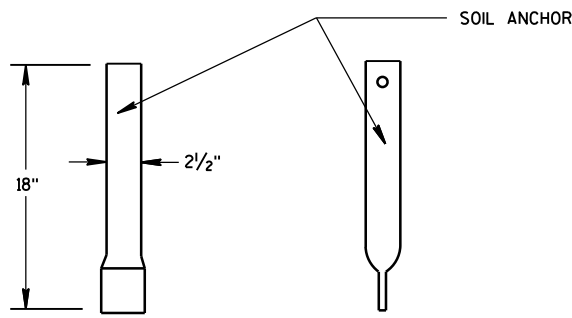
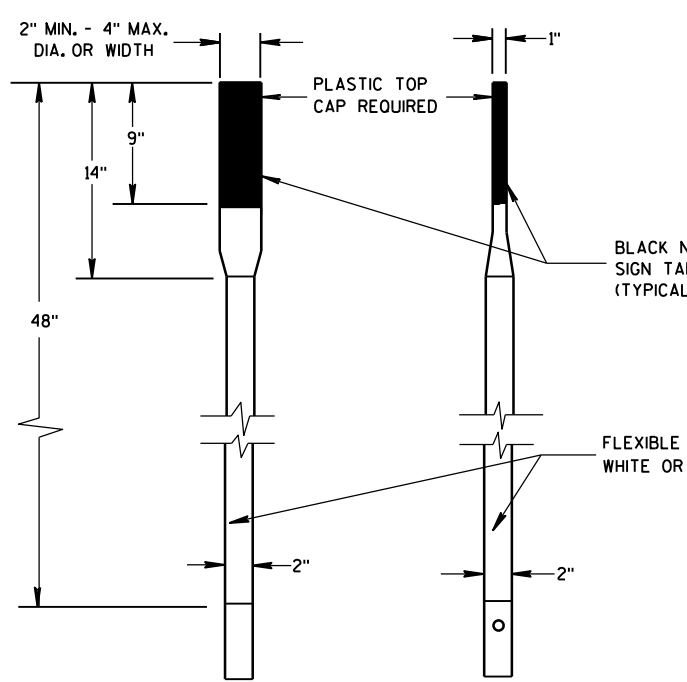
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

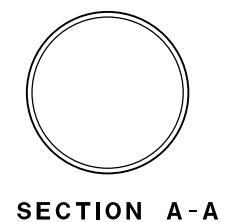
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S.D.D. 15 A 3-2a

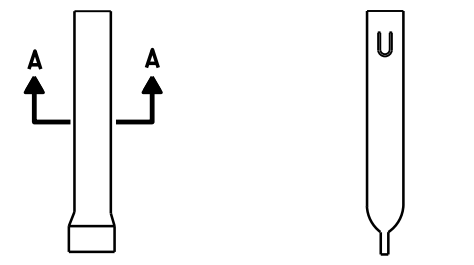
S.D.D. 15 A 3-2a



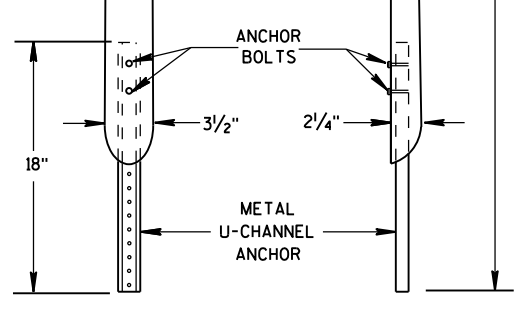
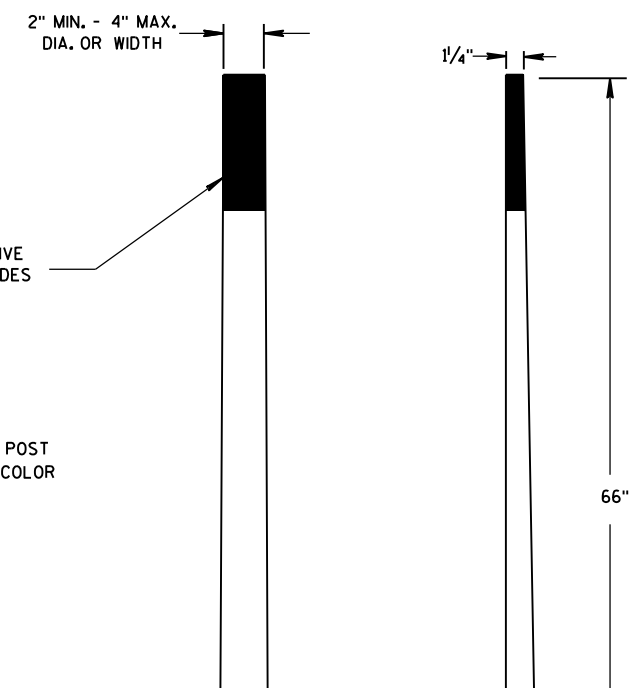
FRONT VIEW SIDE VIEW
ALTERNATE 1



SECTION A-A

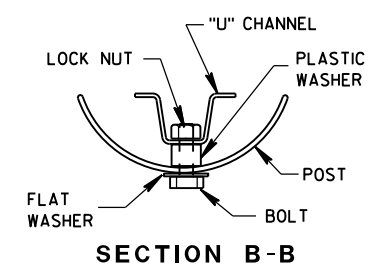


FRONT VIEW SIDE VIEW
ALTERNATE 1

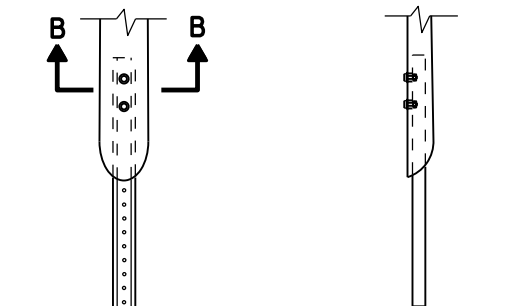


FRONT VIEW SIDE VIEW
ALTERNATE 2

FLEXIBLE MARKER POSTS

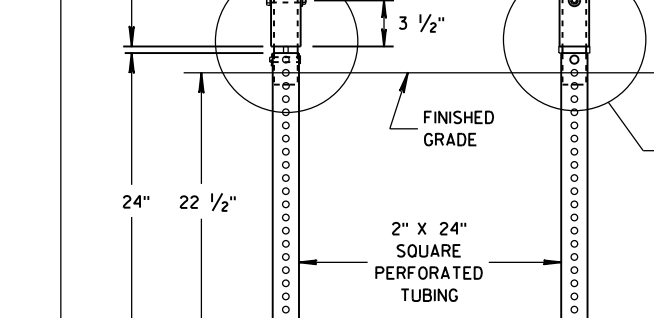
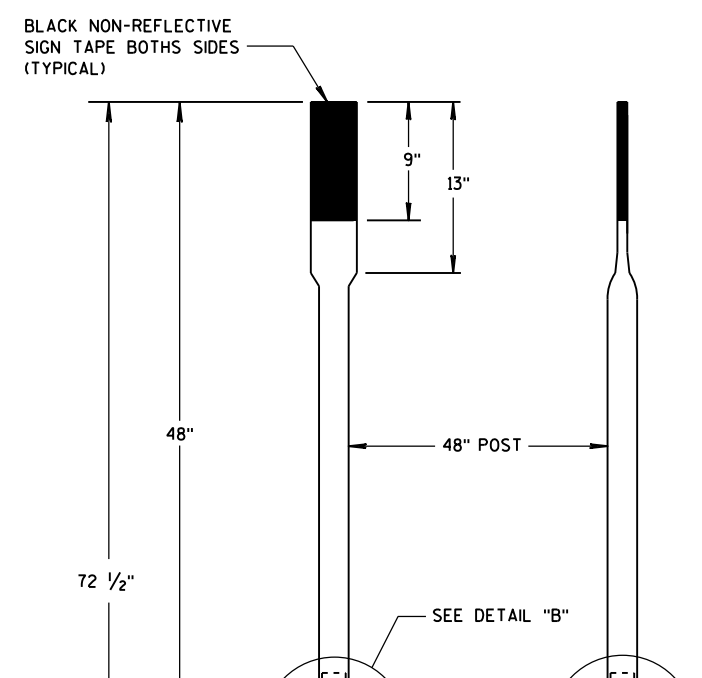


SECTION B-B

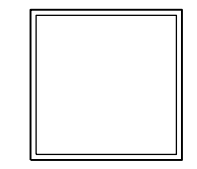


FRONT VIEW SIDE VIEW
ALTERNATE 2

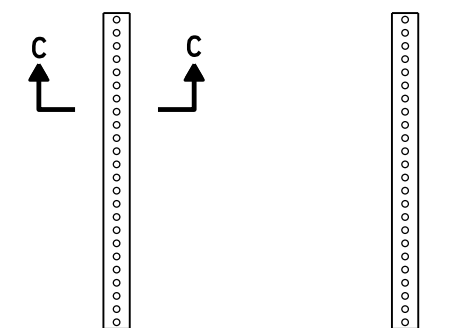
FLEXIBLE MARKER POST ANCHORS



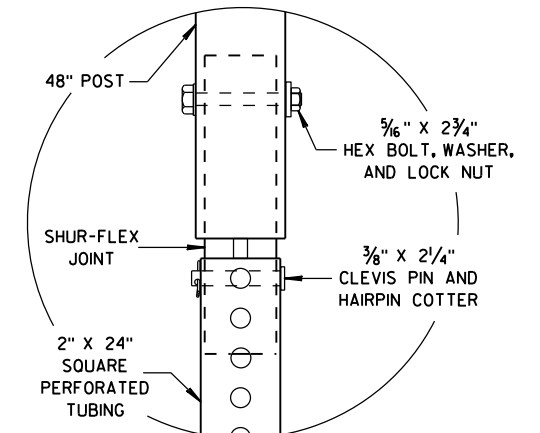
FRONT VIEW SIDE VIEW
ALTERNATE 3



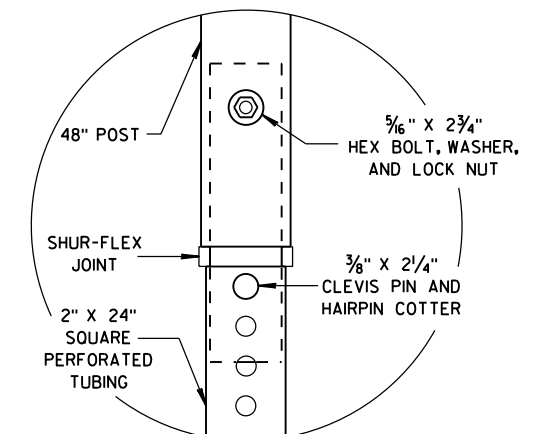
SECTION C-C



FRONT VIEW SIDE VIEW
ALTERNATE 3



DETAIL B

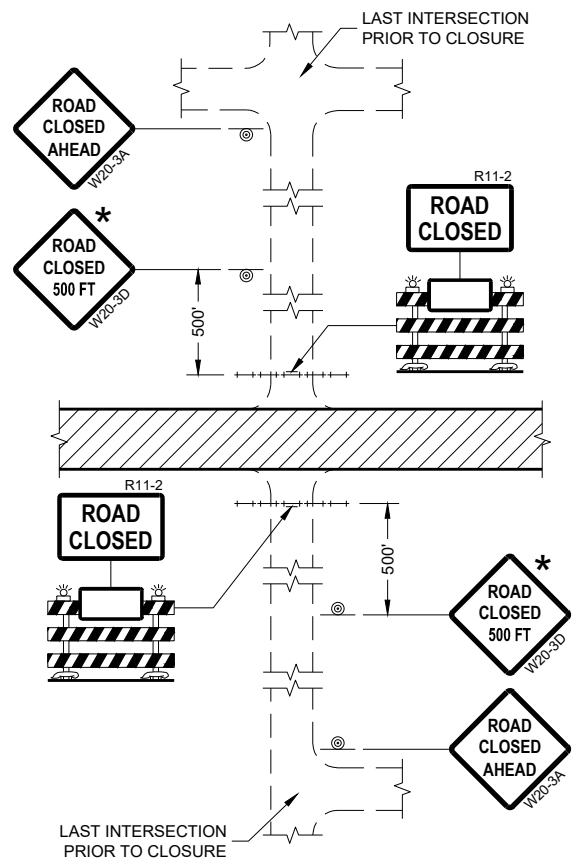


DETAIL C

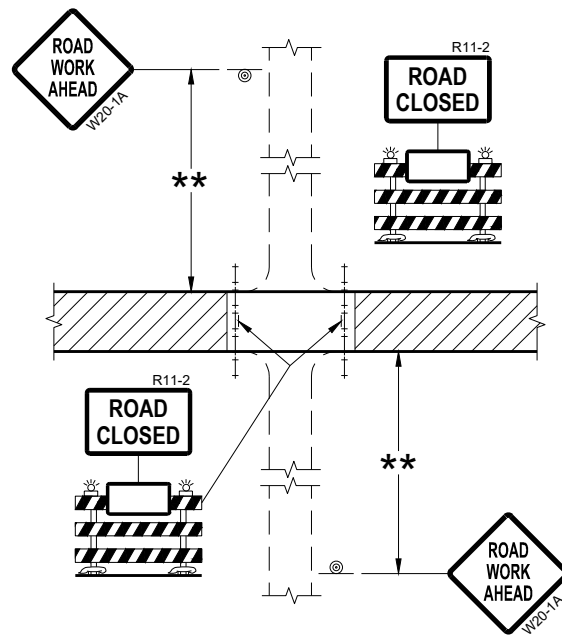
FLEXIBLE MARKER POST FOR CULVERT END

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

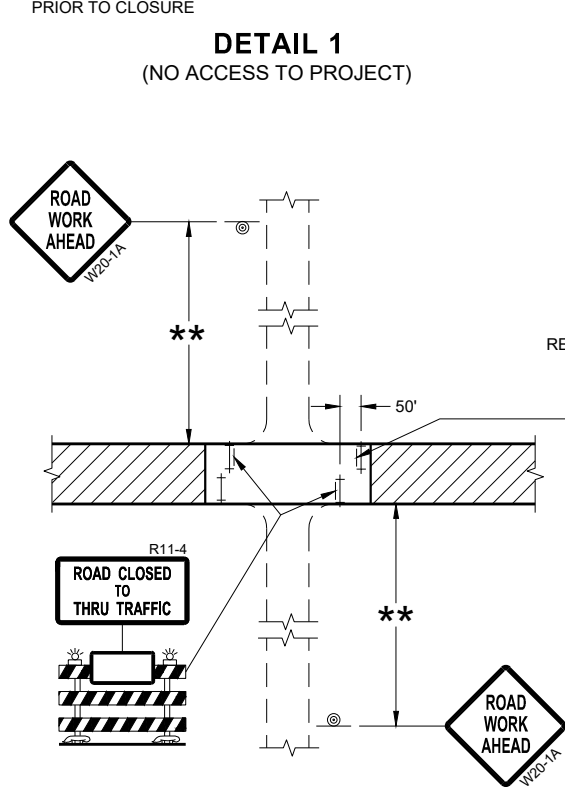
APPROVED
10/1/2012 DATE /S/ Travis Feltes
STATE TRAFFIC ENGINEER OF DESIGN
FHWA



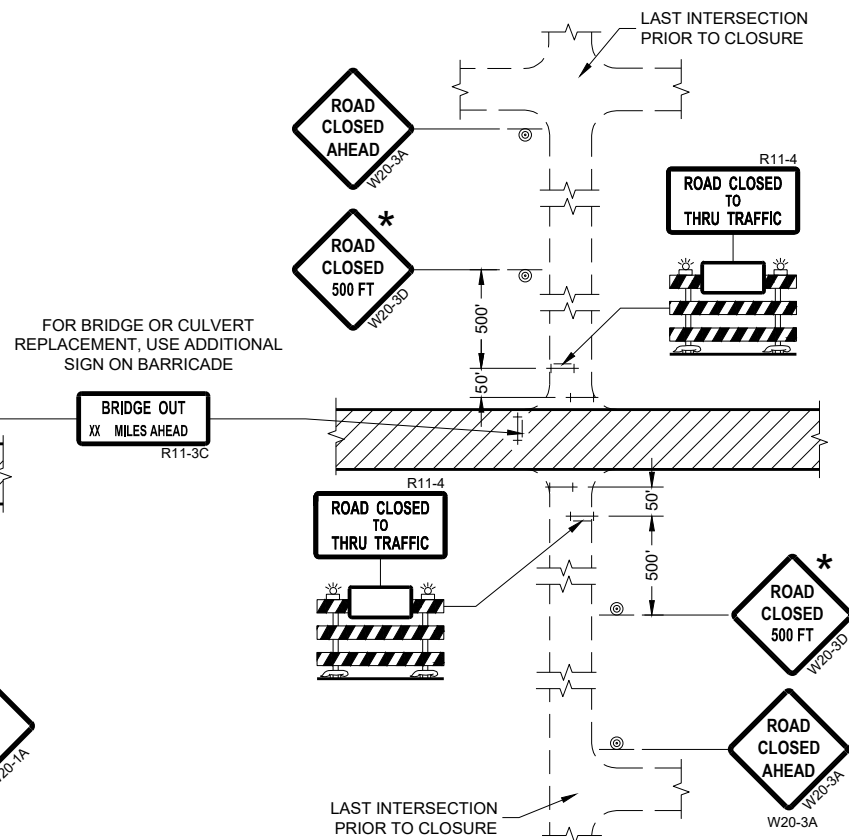
DETAIL 1
(NO ACCESS TO PROJECT)



DETAIL 2
(PUBLIC CROSS-TRAFFIC MAINTAINED.
NO ACCESS TO PROJECT)



DETAIL 3
(PUBLIC CROSS-TRAFFIC MAINTAINED.
CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)



DETAIL 4
(CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE (500 FEET DESIRABLE) TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3, AND R11-4 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
R11-2 SHALL BE 48" X 30".
R11-4 AND R11-3 SHALL BE 60" X 30".

- * OMIT THE "ROAD CLOSED 500 FT." SIGN IF THE LAST INTERSECTION IS 500 FEET OR LESS FROM THE WORK ZONE.
- ** 500' MAX. OR AT LAST INTERSECTION, WHICHEVER IS CLOSEST.

LEGEND

- ⊙ SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- ⚡ TYPE "A" WARNING LIGHT (FLASHING)
- ▨ WORK AREA

**BARRICADES AND SIGNS
FOR
SIDEROAD CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
July 2018 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.




ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

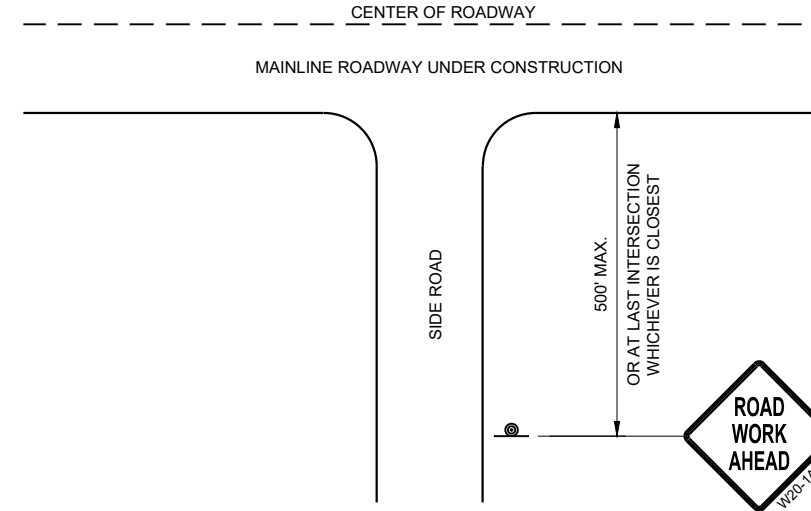
SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

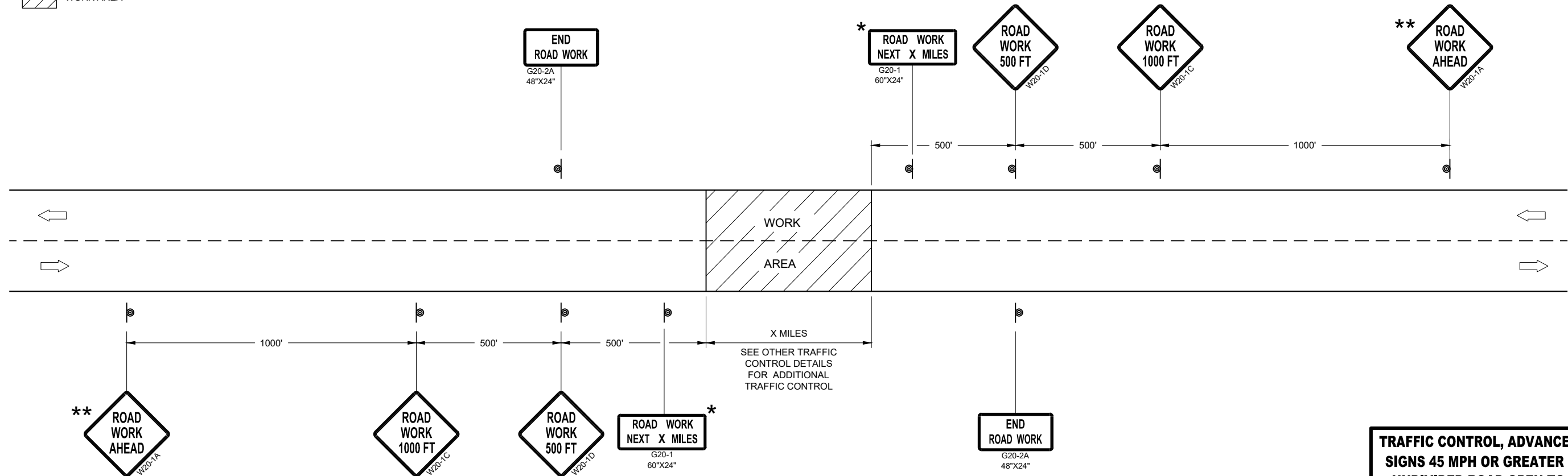
- * OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS
- ** PLACE AN ADDITIONAL W20-1A "ROAD WORK AHEAD" SIGN IF WORK AREA WITHIN THE PROJECT IS SEPARATED BY MORE THAN 2 MILES FROM PREVIOUS WORK AREA.

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC
-  WORK AREA



**TYPICAL SIDE ROAD APPROACH
WARNING SIGN DETAIL**



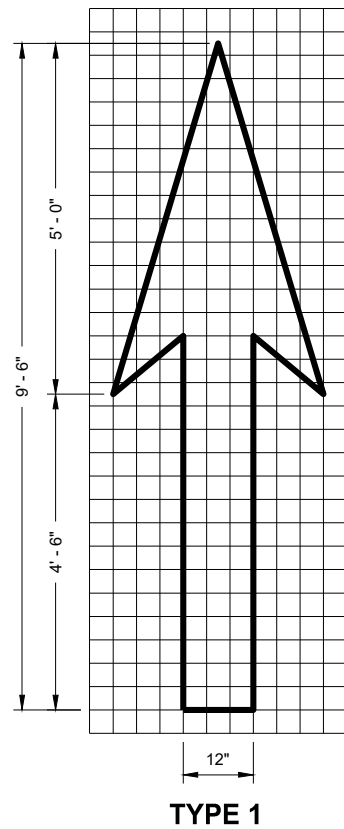
TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45MPH OR GREATER

**TRAFFIC CONTROL, ADVANCE WARNING
SIGNS 45 MPH OR GREATER TWO-WAY
UNDIVIDED ROAD OPEN TO TRAFFIC**

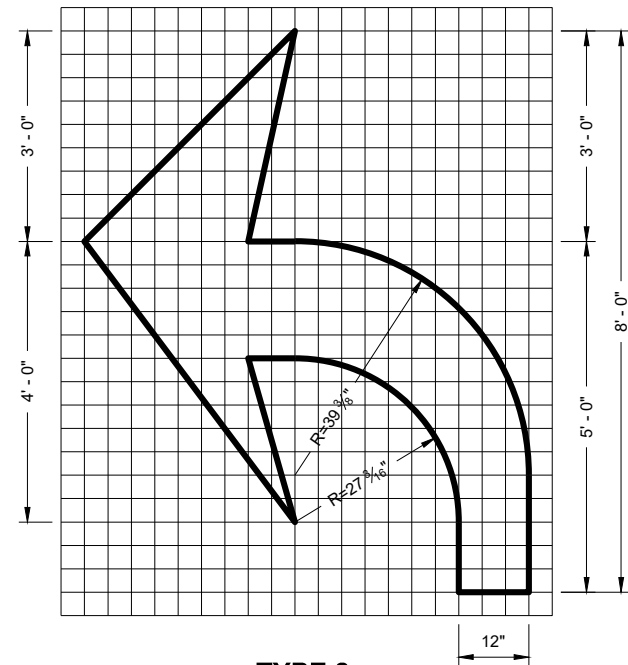
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE July 2018 /S/ Andrew Heidtke
WORK ZONE ENGINEER

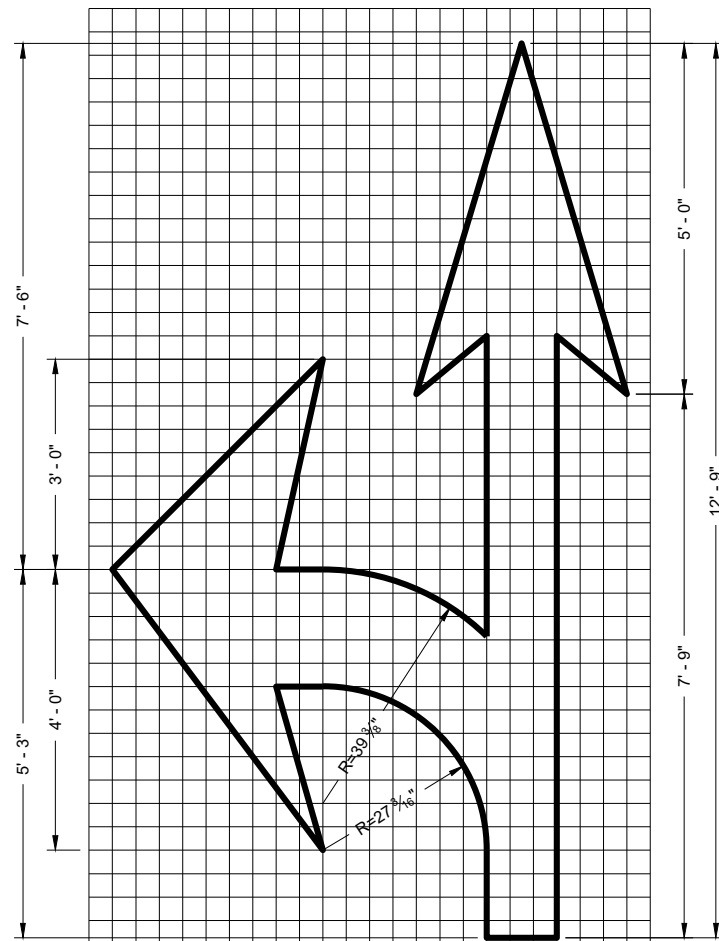
FHWA



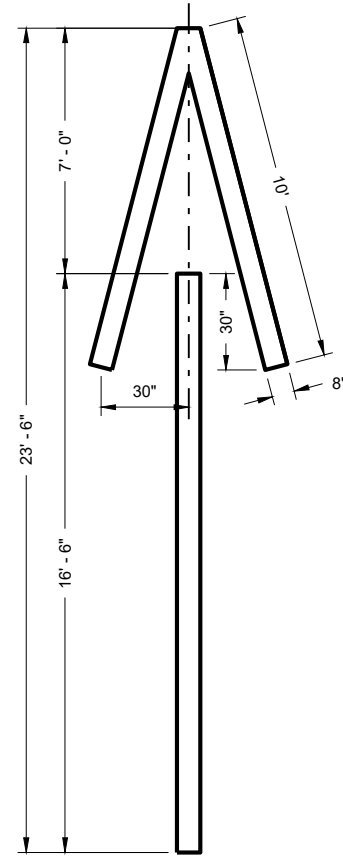
TYPE 1



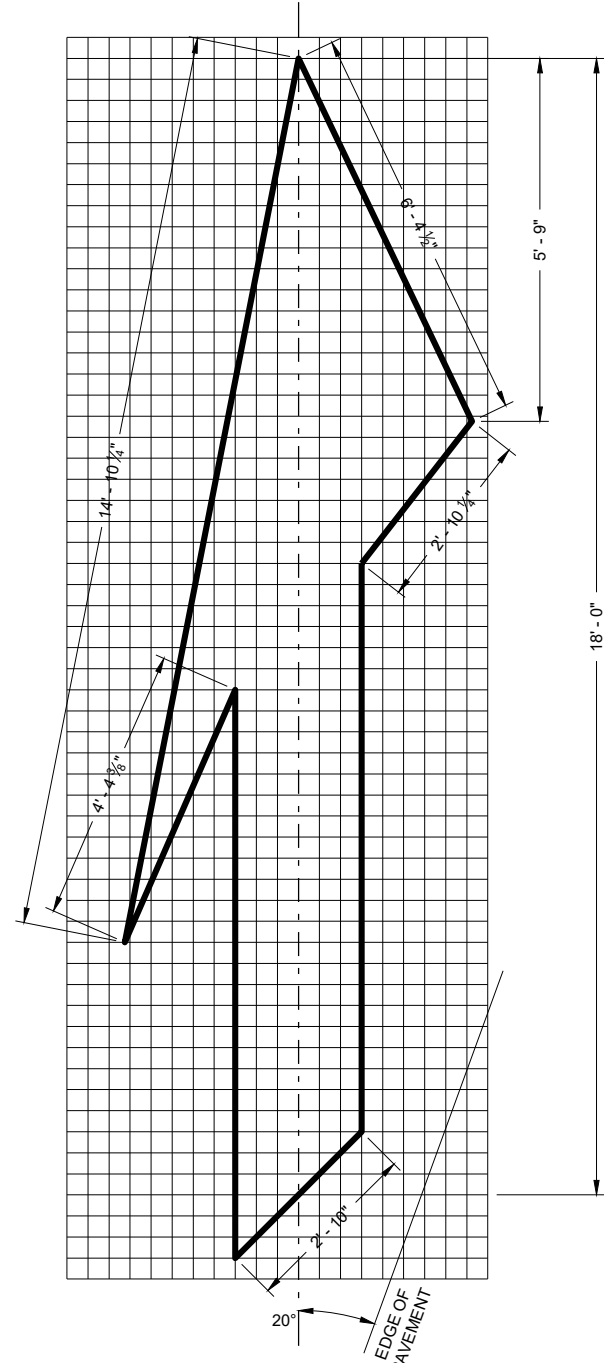
TYPE 2



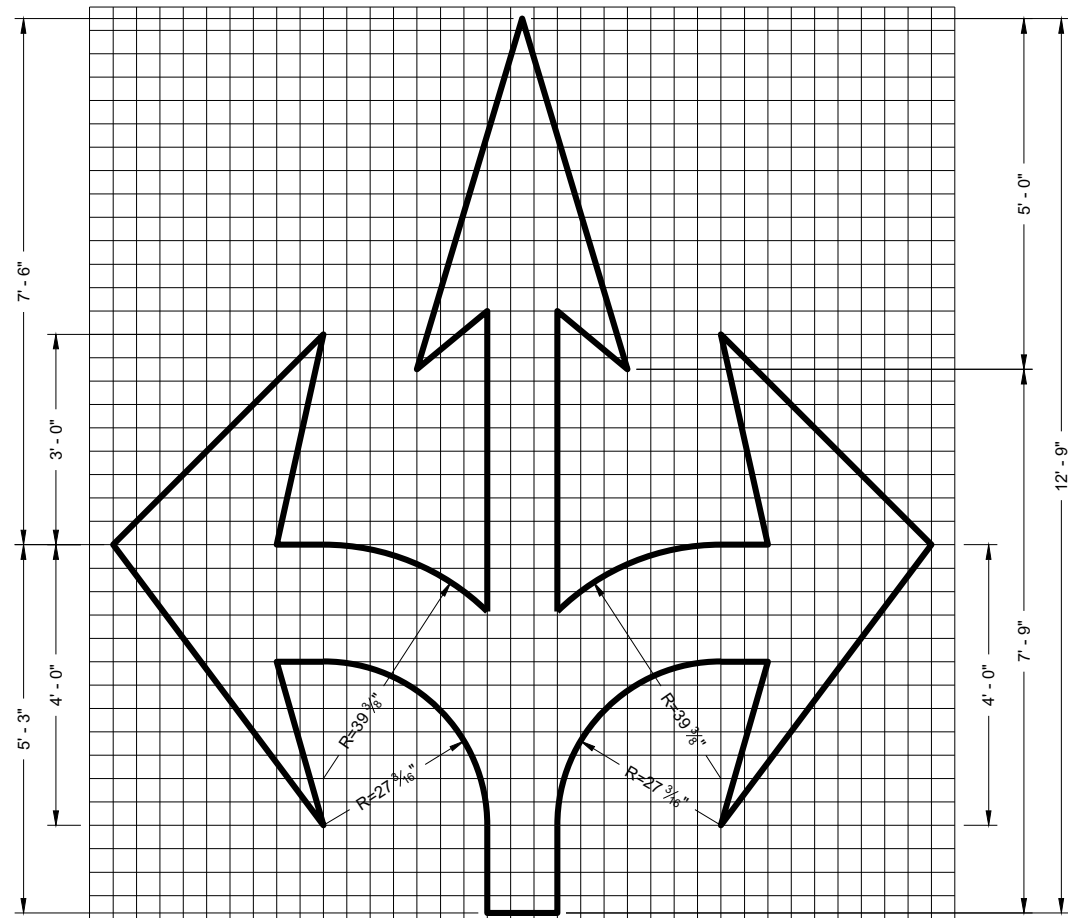
TYPE 3



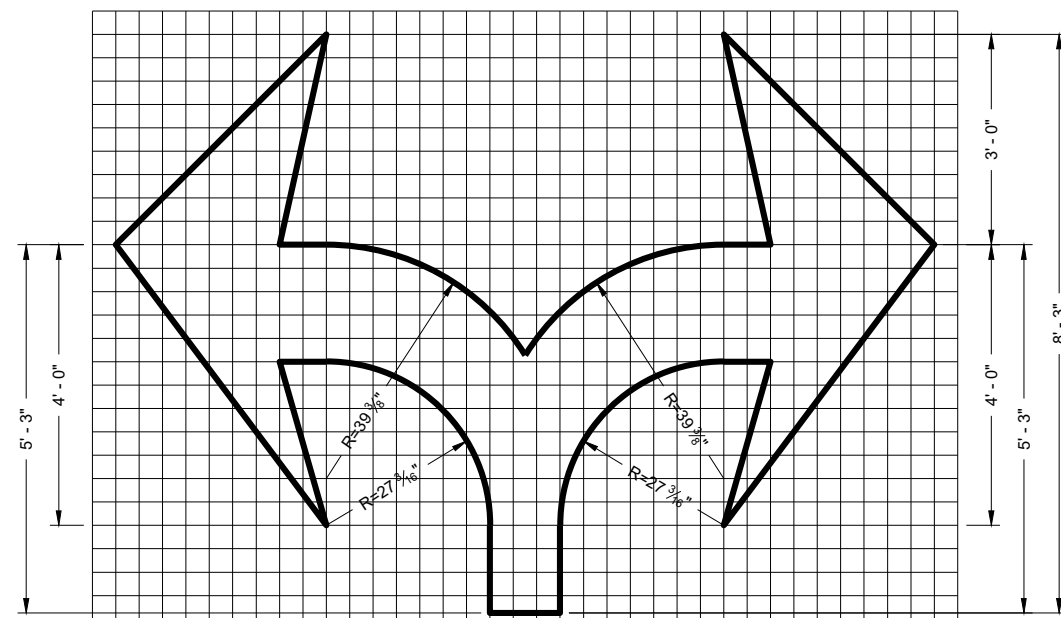
TYPE 4



TYPE 5 LANE DROP ARROW



TYPE 6



TYPE 7

GENERAL NOTES

DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

PAVEMENT MARKING ARROWS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

November 2019

DATE

FHWA




/s/ Matthew Rauch
STATE SIGNING AND MARKING
ENGINEER

GENERAL NOTES

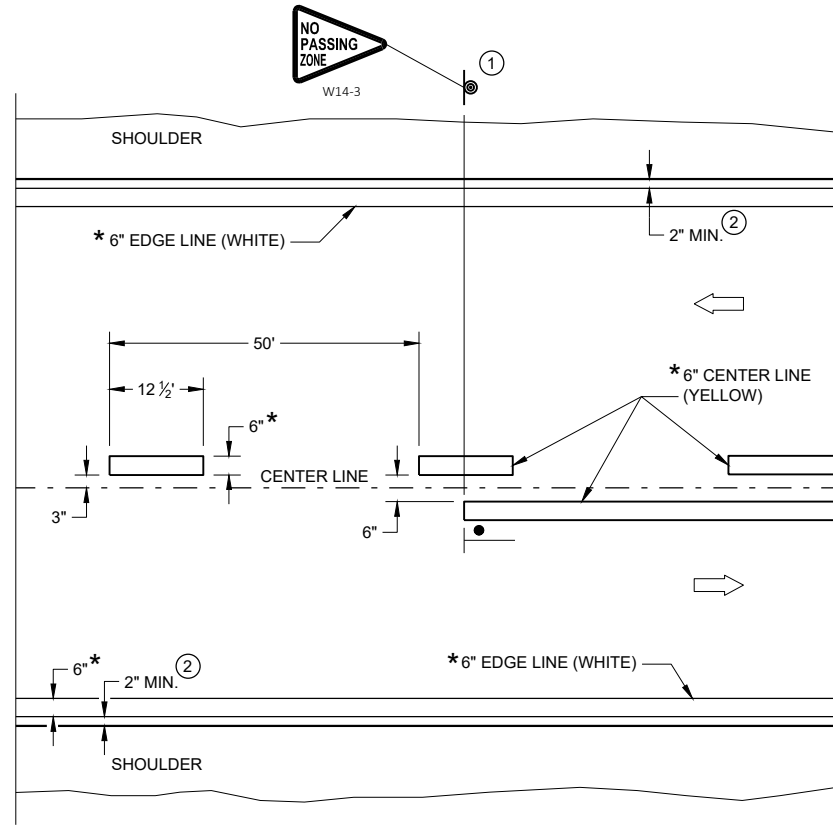
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

- ① LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING
- ② MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

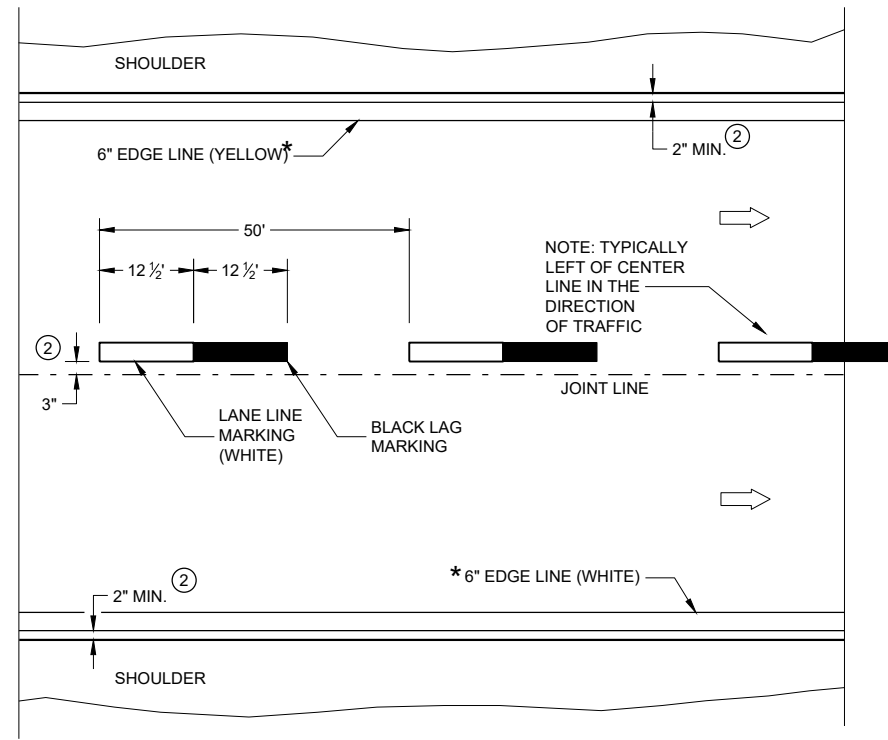
LEGEND

-  "T" MARKING
-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC

* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES



TWO WAY TRAFFIC



ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING

6

6

SDD 15C08-23a

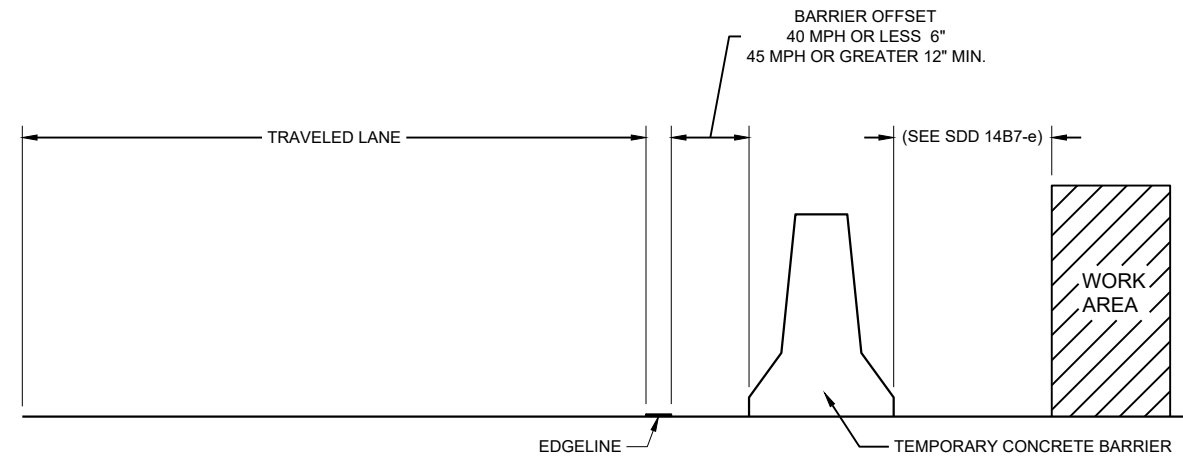
SDD 15C08-23a

PERMANENT LONGITUDINAL PAVEMENT MARKINGS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 /S/ Jeannie Silver
DATE STATEWIDE SIGNING AND MARKING ENGINEER

FHWA



TEMPORARY BARRIER OFFSET FROM EDGELINE

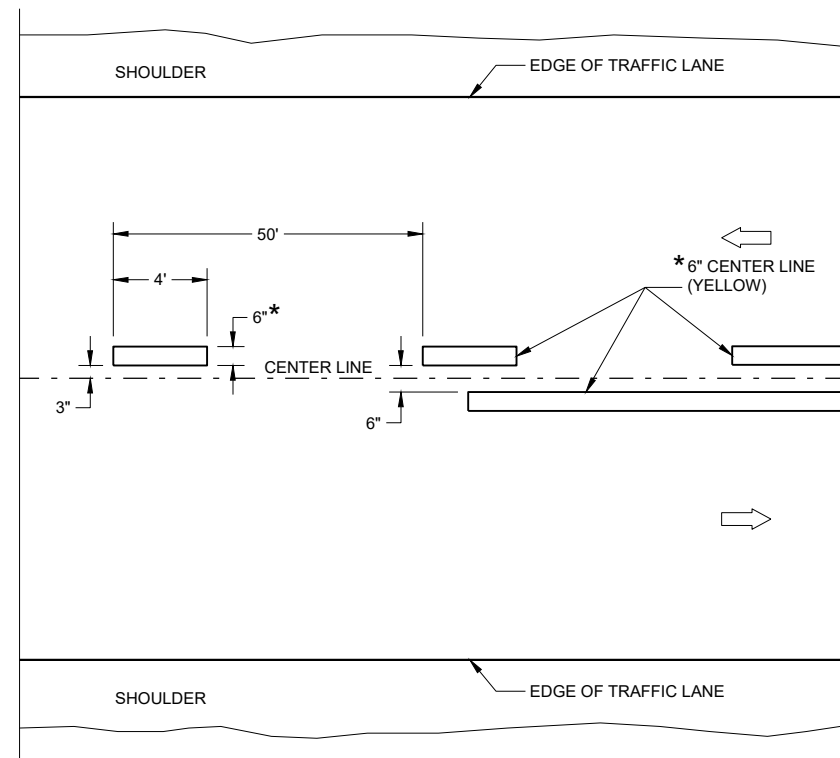
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

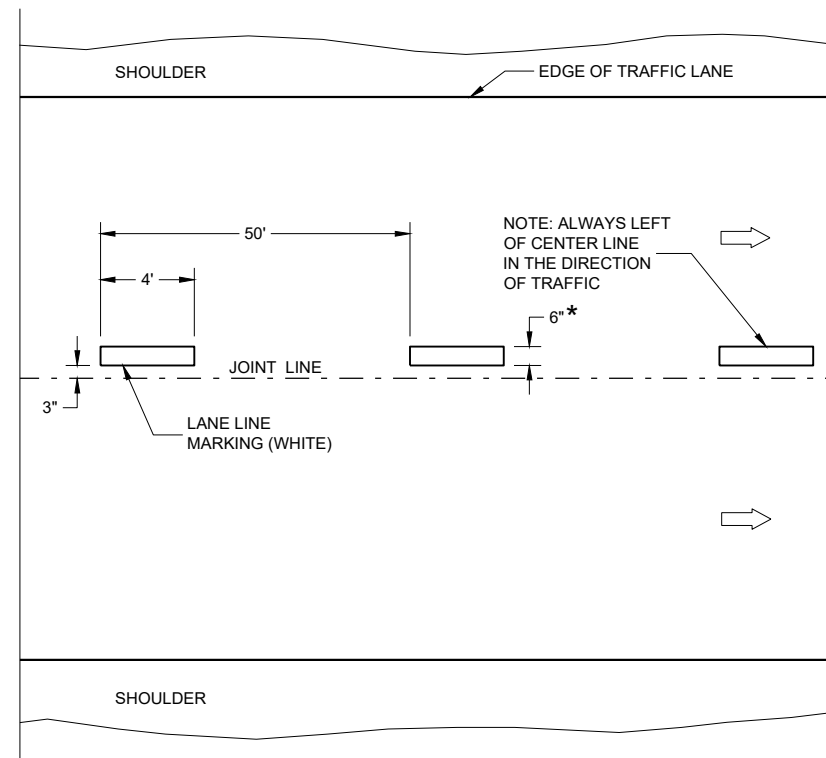
LEGEND

➔ DIRECTION OF TRAFFIC

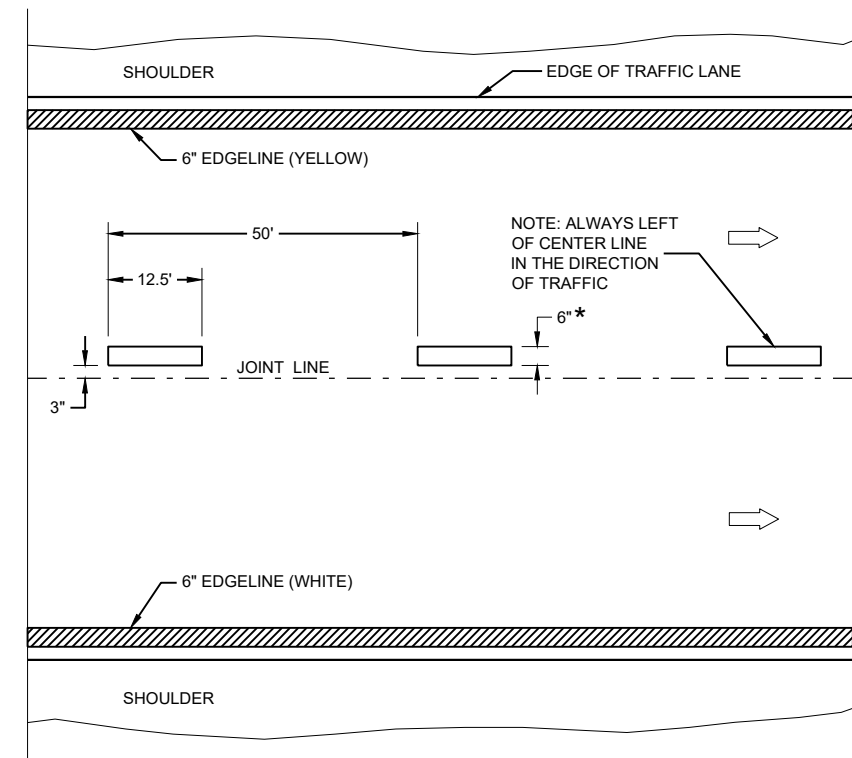
* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES



TWO WAY TRAFFIC



ONE WAY TRAFFIC



FREEWAYS AND EXPRESSWAYS

TEMPORARY PAVEMENT MARKING

TEMPORARY LONGITUDINAL PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 /S/ Jeannie Silver
DATE STATEWIDE SIGNING AND MARKING ENGINEER

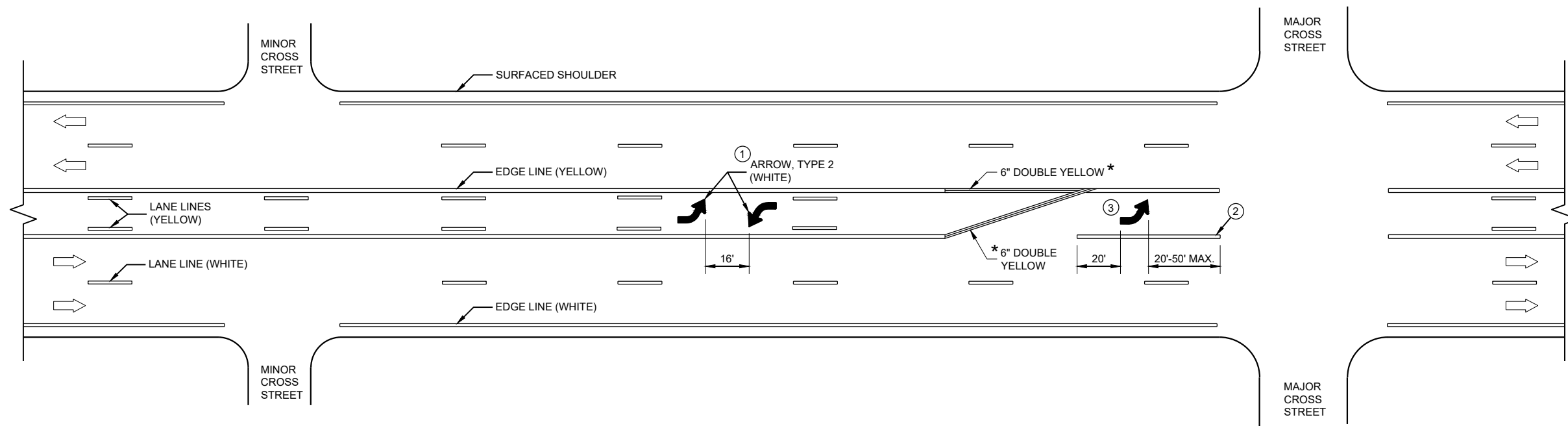
FHWA

GENERAL NOTES

- ① A SET OF ARROWS IS REQUIRED EVERY 400 FEET OR NEAR INTERSECTIONS OR DRIVEWAYS WITH TURNING TRAFFIC.
- ② 10" WHITE
- ③ TURN BAY LENGTH OF LESS THAN 48' DOES NOT REQUIRE PAVEMENT ARROWS OR TEXT.

➡ DIRECTION OF TRAFFIC

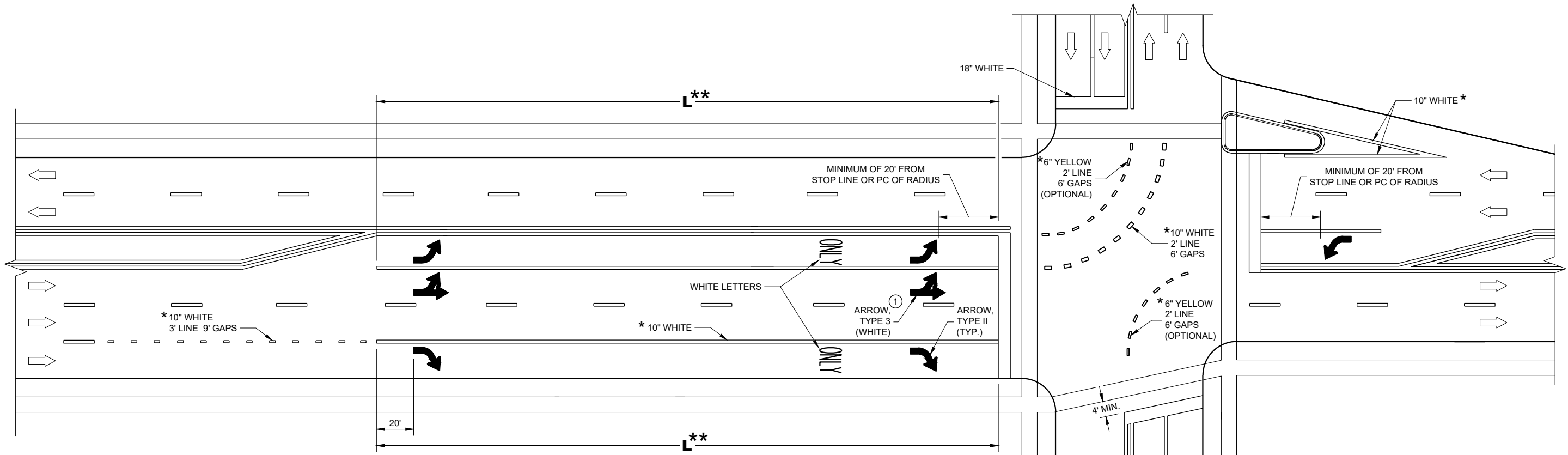
* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES



TWO WAY LEFT TURN LANE

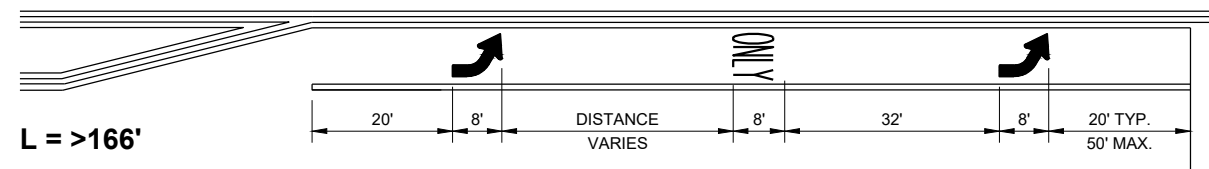
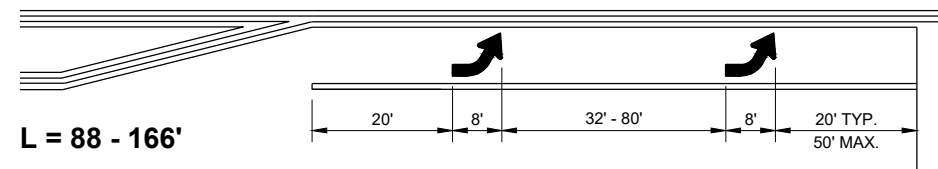
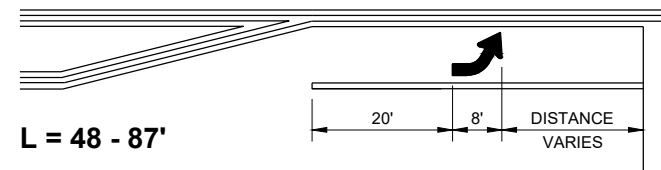
**PAVEMENT MARKING
(TURN LANES)**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



TURN LANE OPTIONS

LENGTH OF TURN BAY (**L**) OF 0 - 47' DOES NOT REQUIRE PAVEMENT MARKING ARROWS OR WORDS



** (SEE TURN LANE OPTIONS FOR PLACEMENT OF PAVEMENT MARKING ARROWS AND WORDS)

GENERAL NOTES

① QUANTITY AND LOCATION OF TYPE 3 ARROWS ARE THE SAME AS THE TYPE II ARROWS IN THE ADJACENT TURN LANE. FOR TURN LANES WITH A PHYSICAL SEPARATION IN THE SAME DIRECTION OF TRAVEL, THE ARROWS AND "ONLY" MARKING MAY BE ELIMINATED.

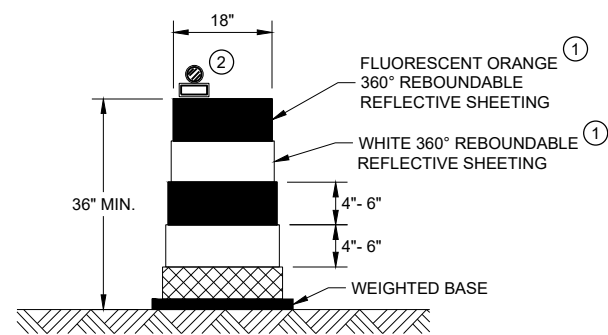
➡ DIRECTION OF TRAFFIC

L = LENGTH OF TURN BAY

* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES

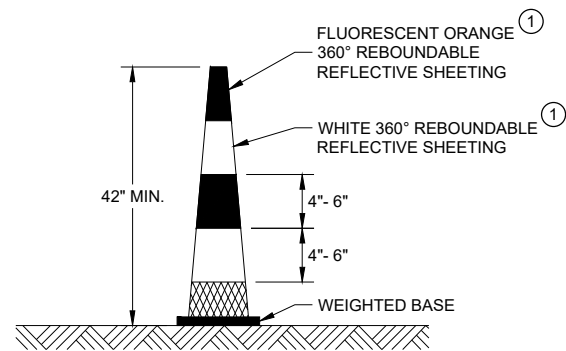
**PAVEMENT MARKING
(TURN LANES)**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



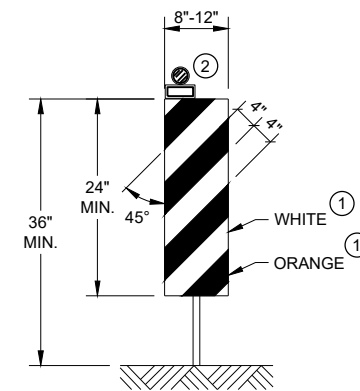
DRUM

BALLAST WIDTHS
RANGE FROM 24"-36"



42" CONE

DO NOT USE IN TAPERS
½ SPACING OF DRUMS
BALLAST WIDTHS
RANGE FROM 14"-20"

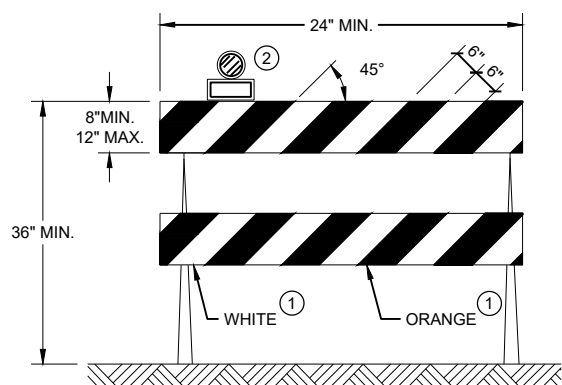


VERTICAL PANEL

THE STRIPES SHALL SLOPE DOWNWARD TO
THE TRAFFIC SIDE FOR CHANNELIZATION.

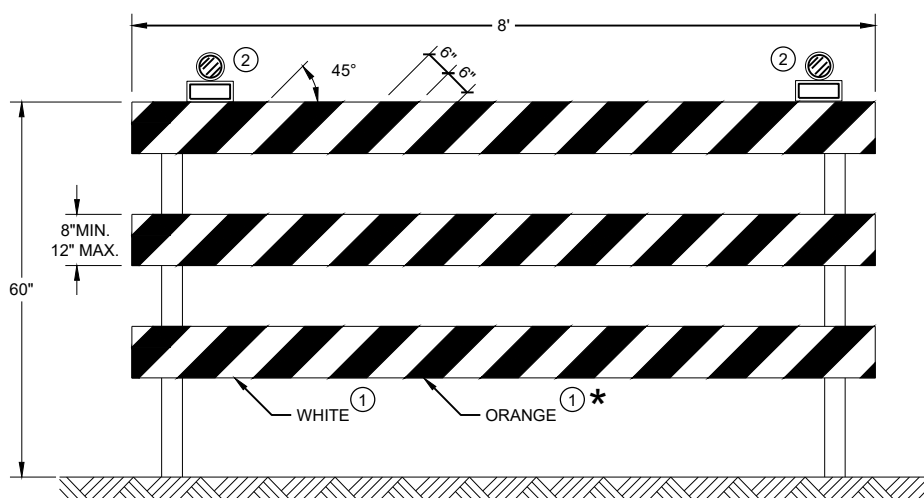
GENERAL NOTES

- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.



TYPE II BARRICADE

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES
MAY BE USED. ALL STRIPES SHALL SLOPE DOWNWARD
TO THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE III BARRICADE

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP
TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.






* IF USED FOR A PERMANENT APPLICATION USE RED SHEETING.

**CHANNELIZING DEVICES
DRUMS, CONES, BARRICADES
AND VERTICAL PANELS**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
 November 2022 /S/ Andrew Heidtke
 DATE WORK ZONE ENGINEER
 FHWA

LEGEND

-  SIGN ON PORTABLE OR PERMANENT SUPPORT
-  TEMPORARY PORTABLE RUMBLE STRIP ARRAY
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS, DEVICES, AND LOCATION OF ALL FLAGGERS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGING

FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT REMOVE TEMPORARY PORTABLE RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.

- ① FOR MOVING WORK OPERATIONS, POST ADDITIONAL W20-7A FLAGGER SIGNS AT APPROXIMATELY 3,500' INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.
- ② SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

WHEN THE DISTANCE BETWEEN FLAGGERS EXCEEDS 2 MILES, A PILOT CAR IS REQUIRED. WHEN CURVES REDUCE SIGHT DISTANCE BELOW 400', A PILOT CAR IS REQUIRED.

TEMPORARY PORTABLE RUMBLE STRIPS

UTILIZE TEMPORARY PORTABLE RUMBLE STRIPS ON ALL FLAGGING OPERATIONS.

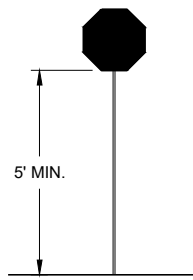
- ③ EACH TEMPORARY PORTABLE RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS PLACED TRANSVERSE ACROSS THE LANE AT THE LOCATIONS SHOWN. WITHIN EACH ARRAY, SPACING BETWEEN RUMBLE STRIPS SHALL BE 15 FEET ON CENTER

ONLY USE TEMPORARY PORTABLE RUMBLE STRIPS FROM THE APPROVED PRODUCTS LIST.

INSTALL TEMPORARY RUMBLE STRIPS PER MANUFACTURER'S RECOMMENDATIONS.

PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.

DO NOT INSTALL TEMPORARY PORTABLE RUMBLE STRIPS ON GRAVEL, MILLED SURFACES, OR ASPHALT THAT HAS BEEN PAVED LESS THAN 12 HOURS.



STOP/SLOW PADDLE ON SUPPORT STAFF

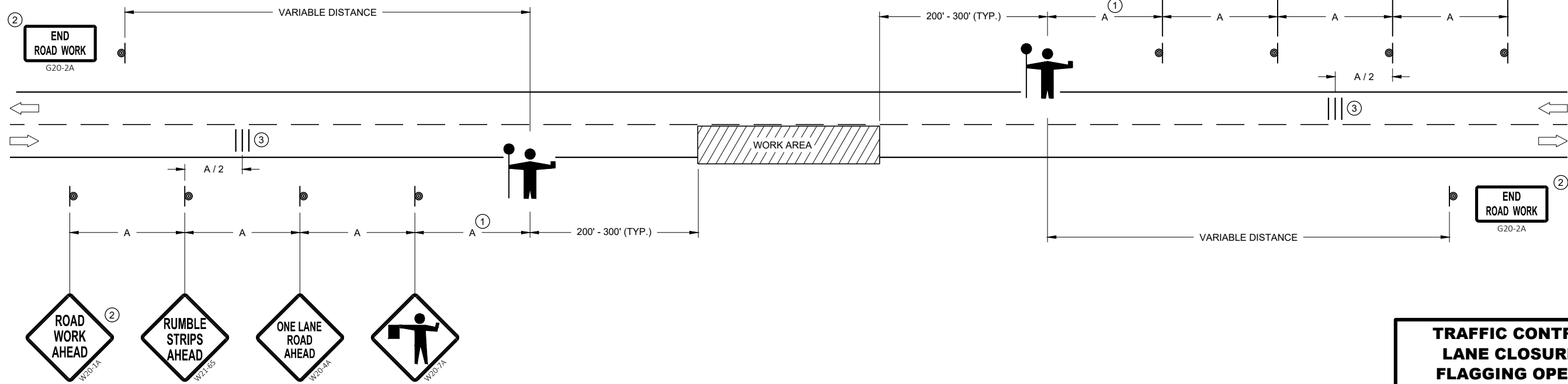
SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE

SPEED LIMIT	SPACING "A"
25-30 MPH	200'
35-40 MPH	350'
45-55 MPH	500'



W03-4

USE OF W03-4 SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7A AND W20-4A SIGNS, USING SPACING "A".










TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE: May 2022 /S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

GENERAL NOTES

-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL CONE 42-INCH
-  TRAFFIC CONTROL DRUM
-  TEMPORARY PORTABLE RUMBLE STRIP ARRAY
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  **AFAD** AUTOMATED FLAGGER ASSISTANCE DEVICE (AFAD)

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS, DEVICES, AND LOCATION OF ALL FLAGGERS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGING

IF THE AUTOMATED FLAGGER ASSISTANCE DEVICE (AFAD) STOPS WORKING, FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT REMOVE TEMPORARY PORTABLE RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.

WHEN THE DISTANCE BETWEEN FLAGGERS EXCEEDS 2 MILES, A PILOT CAR IS REQUIRED. WHEN CURVES REDUCE SIGHT DISTANCE BELOW 400', A PILOT CAR IS REQUIRED.

- ① SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.
- ② IF FLAGGERS ARE PHYSICALLY NEEDED TO FLAG, REPLACE WO3-4 SIGNS WITH W20-7A SIGNS.

TEMPORARY PORTABLE RUMBLE STRIPS

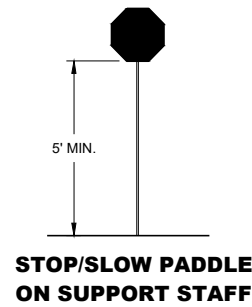
UTILIZE TEMPORARY PORTABLE RUMBLE STRIPS ON ALL FLAGGING OPERATIONS.

ONLY USE TEMPORARY PORTABLE RUMBLE STRIPS FROM THE APPROVED PRODUCTS LIST.

PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.

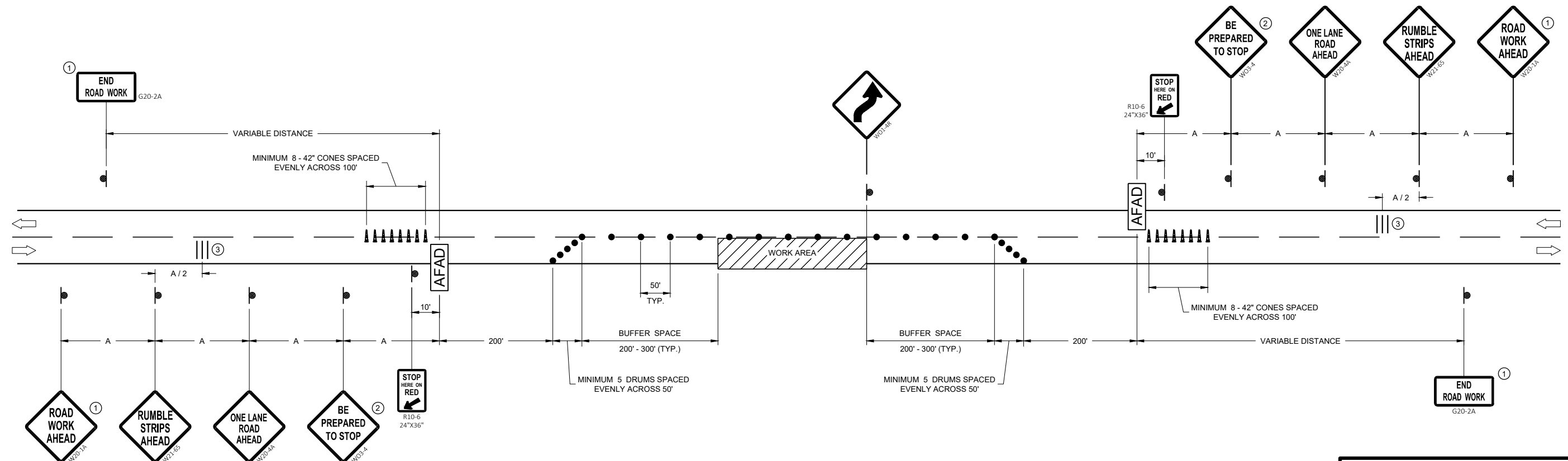
DO NOT INSTALL TEMPORARY PORTABLE RUMBLE STRIPS ON GRAVEL, MILLED SURFACES, OR ASPHALT THAT HAS BEEN PAVED LESS THAN 12 HOURS.

③ EACH TEMPORARY PORTABLE RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS PLACED TRANSVERSELY AT THE LOCATIONS SHOWN. WITHIN EACH ARRAY, SPACING BETWEEN RUMBLE STRIPS SHALL BE 15 FEET ON CENTER.



SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE

SPEED LIMIT	SPACING "A"
25-30 MPH	200'
35-40 MPH	350'
45-55 MPH	500'







TRAFFIC CONTROL, LANE CLOSURE WITH AUTOMATED FLAGGER ASSISTANCE DEVICE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2022 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA

LEGEND

- V1 LEAD VEHICLE
- V2 MARKING VEHICLE
- V3 SHADOW VEHICLE
-  TRUCK MOUNTED ATTENUATOR (TMA)
-  SIGN ON TEMPORARY SUPPORT
-  DIRECTION OF TRAFFIC
-  FLASHING ARROW PANEL (CAUTION)

GENERAL NOTES

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

ALL VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL OPERATING IN CAUTION MODE. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.

DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL AND HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.

THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS.

WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR LAY STATIONARY SIGNS AND SUPPORTS FLAT ON THE GRADE WITH UPRIGHTS ORIENTED PARALLEL TO AND DOWNSTREAM FROM TRAFFIC.

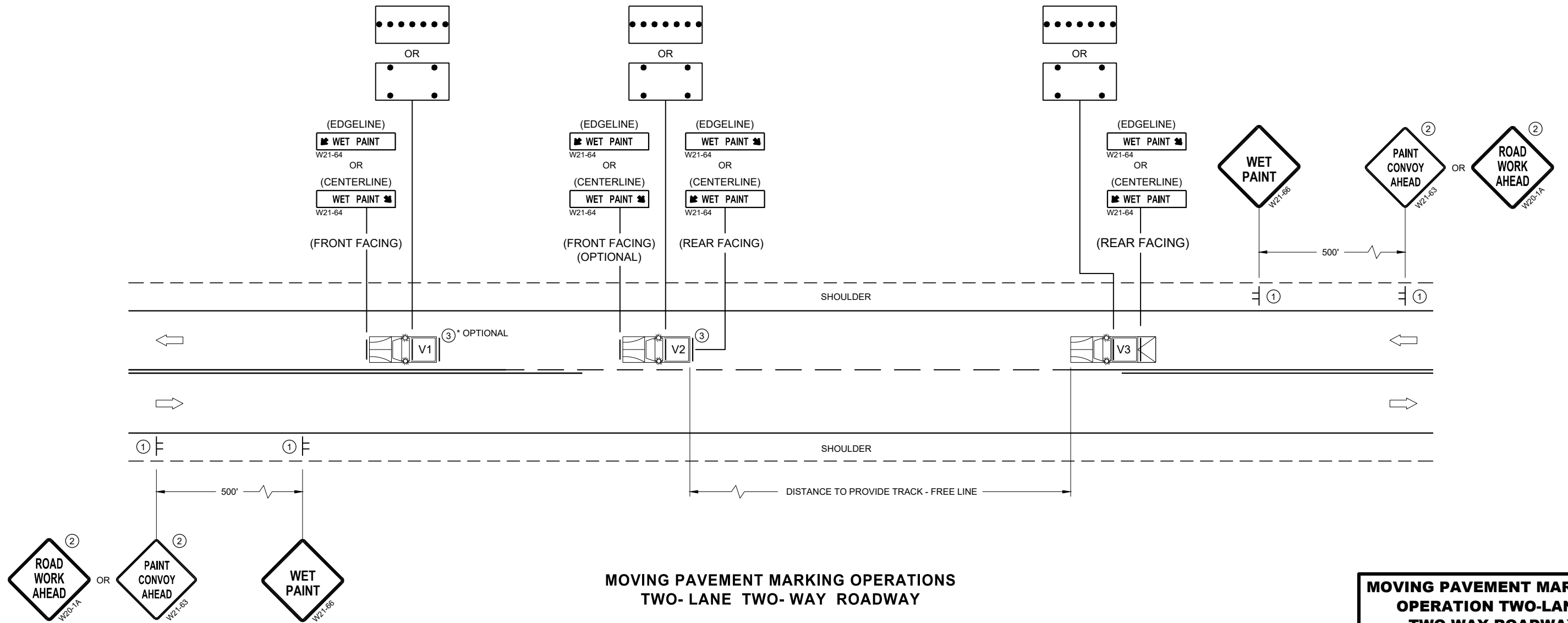
CONES SHOULD BE USED BETWEEN THE MARKING AND SHADOW VEHICLE AT 100 FOOT SPACING. CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.

CONES SHALL BE A MINIMUM OF 28" FOR WET PAVEMENT MARKING.

- ① SIGNS SHALL BE REPEATED APPROXIMATELY EVERY THREE MILES.
- ② IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1A OR W21-63 ARE NOT REQUIRED.
- ③ V1 AND V2 CAN BE SWITCHED SO THAT THE MARKER IS THE LEAD VEHICLE.

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**MOVING PAVEMENT MARKING OPERATIONS
TWO-LANE TWO-WAY ROADWAY**

SDD 15C19-08a

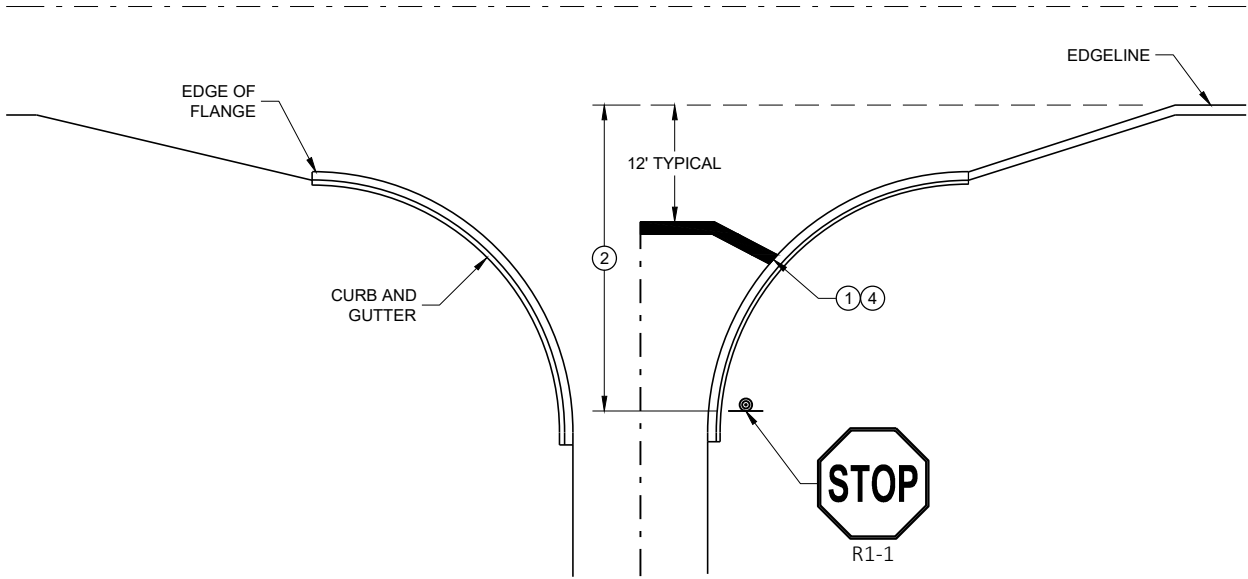
SDD 15C19-08a

MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED February 2023 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

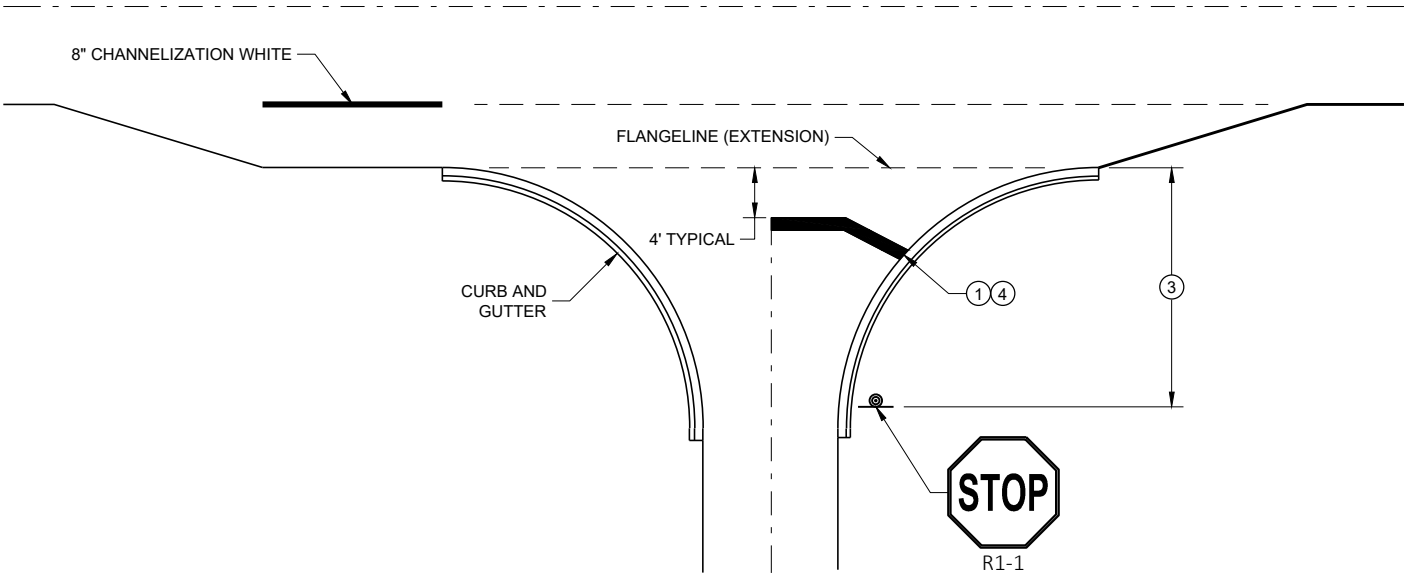
GENERAL NOTES

STOP SIGN SHALL BE PLACED A MINIMUM OF 6 FEET TO A MAXIMUM OF 50 FEET FROM THE EDGELINE LOCATION.

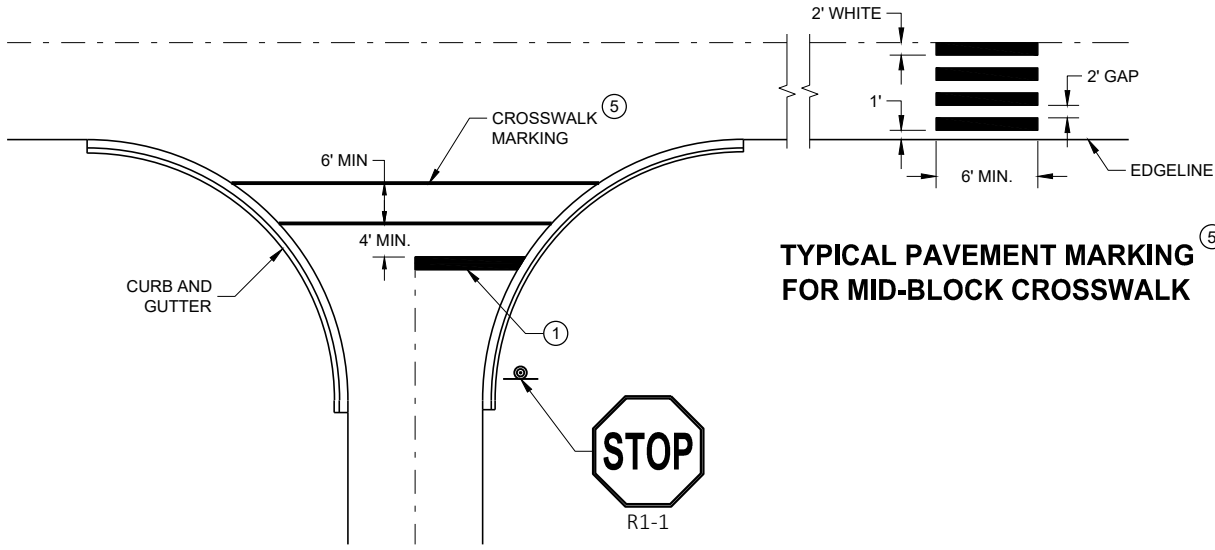
- ① 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE REGION MARKING ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
- ② NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGELINE.
- ③ NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGELINE EXTENSION.
- ④ MOVE CLOSER TO THE EDGE OF TRAVEL LINE AS NEEDED FOR VISIBILITY AND SIGHT LINES (NO CLOSER THAN 4 FEET).
- ⑤ LADDER BAR CROSSWALKS SHOULD ONLY BE USED FOR MID BLOCK CROSSINGS. USE 2 - 6" TRANSVERSE LINES INSTEAD.



TYPICAL STOP LINE PAVEMENT MARKING WITH CURB AND GUTTER

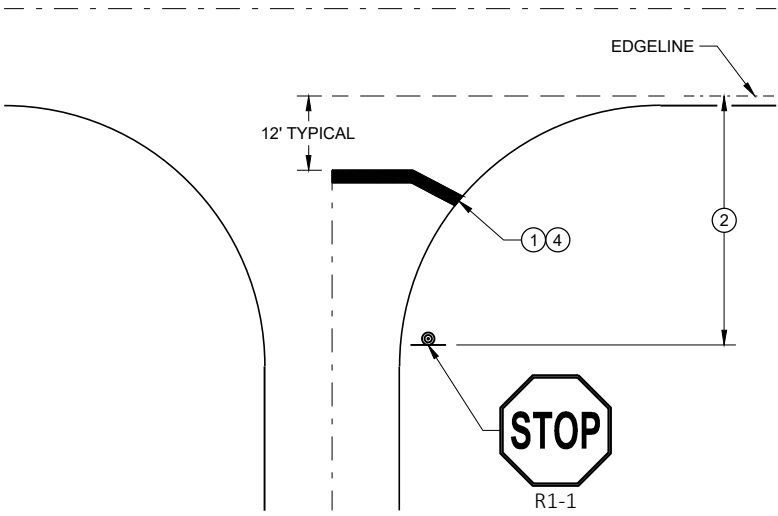


TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH RIGHT TURN LANE



TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH CROSSWALK MARKING

TYPICAL PAVEMENT MARKING FOR MID-BLOCK CROSSWALK



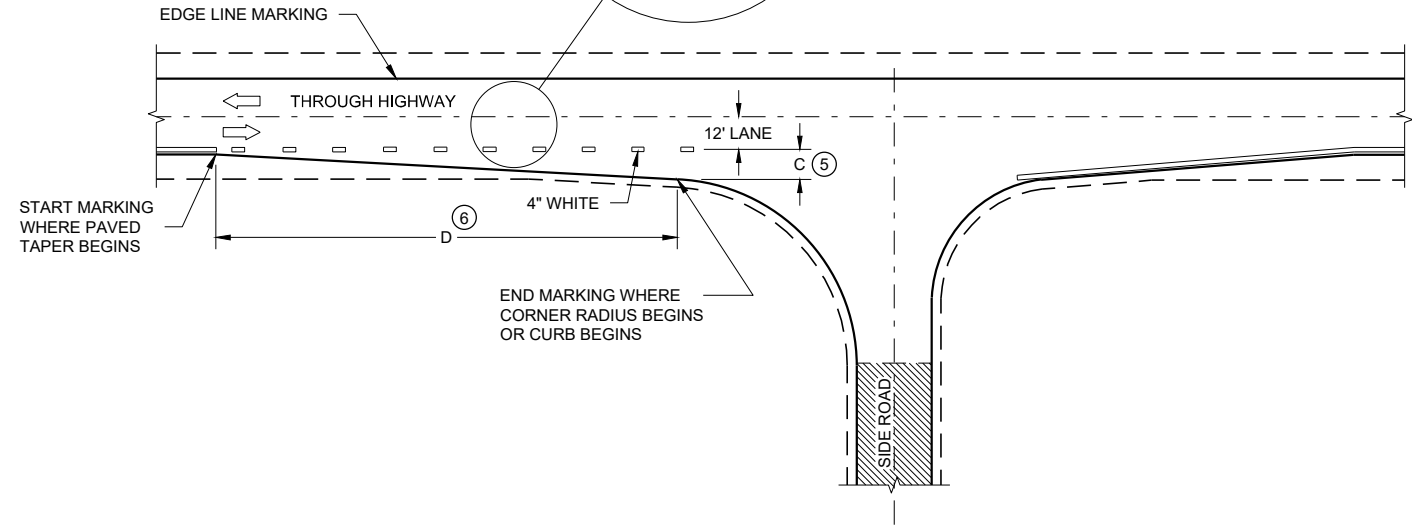
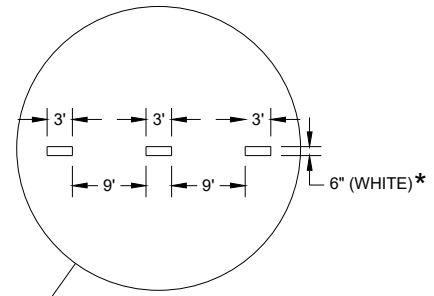
TYPICAL STOP LINE PAVEMENT MARKING WITHOUT CURB AND GUTTER

STOP LINE AND CROSSWALK PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2019 /S/ Matthew Rauch
DATE STATE SIGNING AND MARKING ENGINEER

FHWA



MINOR INTERSECTION

* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES

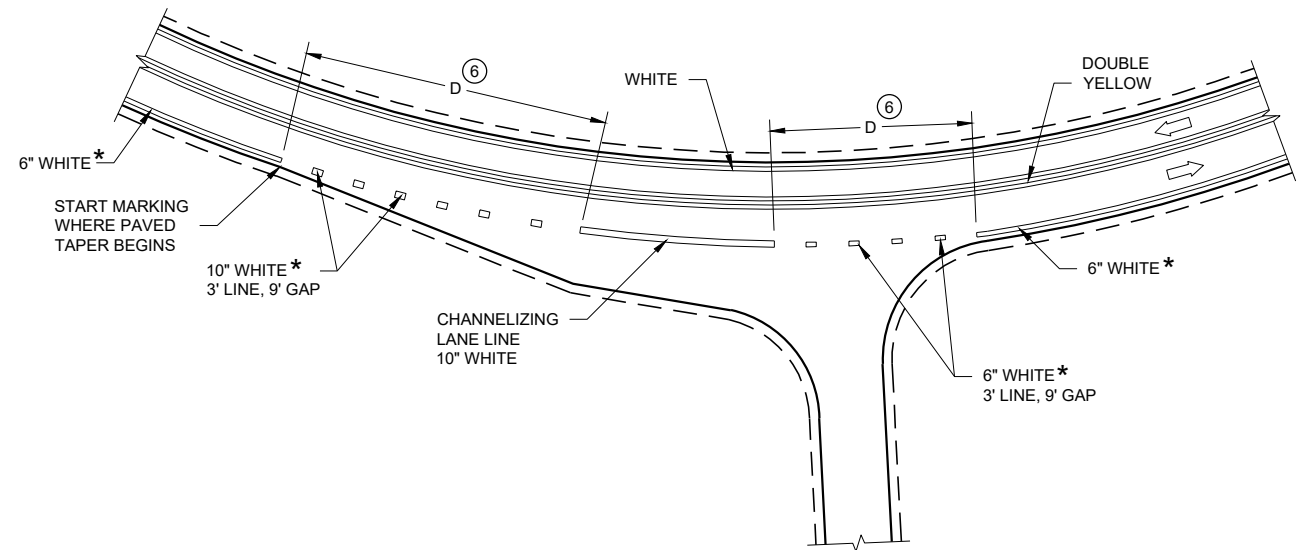
GENERAL NOTES

OMIT EDGE LINES THROUGH INTERSECTIONS. CONTINUE EDGE LINES THROUGH DRIVEWAYS.

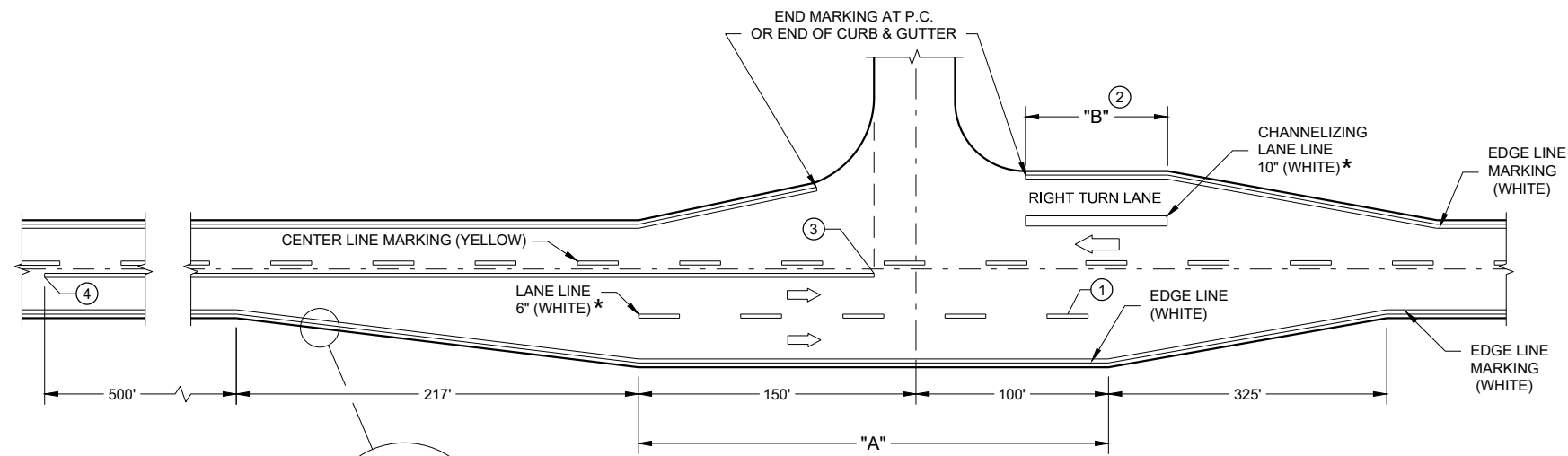
- ① WHEN DISTANCE "A" IS LESS THAN 250 FEET, OMIT LANE LINE.
- ② WHEN DISTANCE "B" IS LESS THAN 100 FEET, OMIT CHANNELIZING LANE LINE.
- ③ BARRIER LINE ENDS AT SIDE ROAD PAVEMENT / SURFACE EDGE EXTENSION.
- ④ BARRIER LINE STARTS 500 FEET PRIOR TO THE BYPASS TAPER.
- ⑤ WHEN DISTANCE "C" IS LESS THAN 4 FEET, OMIT DOTTED EXTENSION.
- ⑥ WHEN DISTANCE "D" IS LESS THAN 50 FEET, OMIT DOTTED EXTENSION.

LEGEND

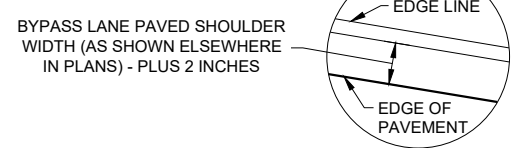
➔ DIRECTION OF TRAVEL



INTERSECTION ON OUTSIDE OF CURVE



**MAJOR INTERSECTIONS
(INTERSECTION WITH FULL RIGHT TURN LANE OR BYPASS LANE)**







BYPASS LANE PAVED SHOULDER WIDTH (AS SHOWN ELSEWHERE IN PLANS) - PLUS 2 INCHES

**PAVEMENT MARKING
(INTERSECTIONS)**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  DIRECTION OF TRAFFIC
-  WORK ZONE

GENERAL NOTES

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" SIGNS MAY BE USED IF APPROVED BY THE REGIONAL TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

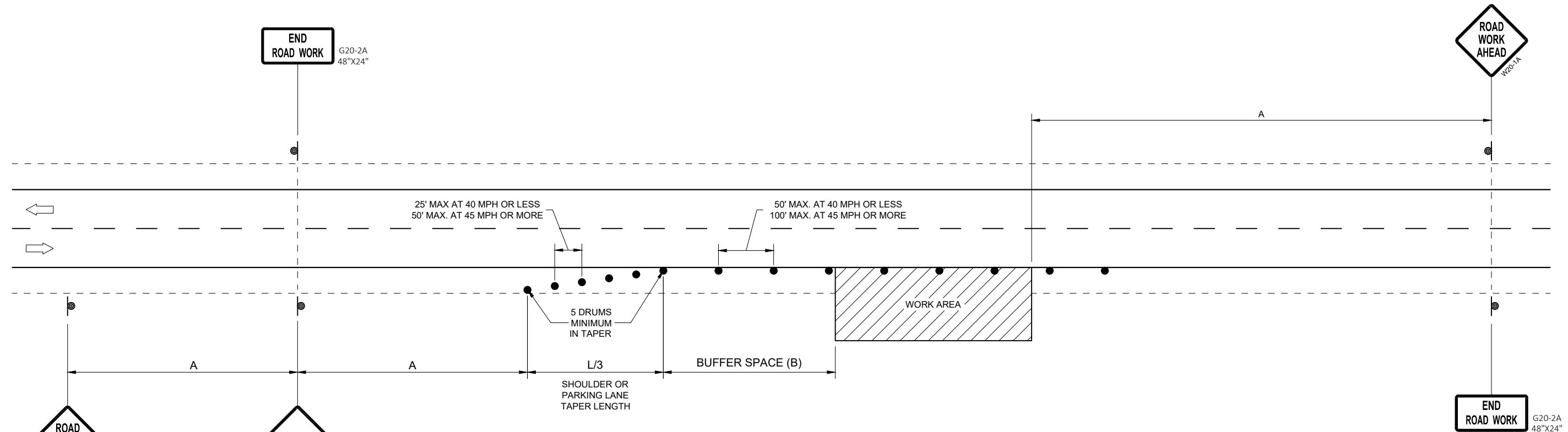
CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

W20-1A AND G20-2A SIGNS ARE NOT REQUIRED IF THE WORK AREA IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT. G20-2A SIGNS MAY ALSO BE OMITTED IF DURATION OF WORK IS LESS THAN 7 CONTINUOUS DAYS AND NIGHTS.

6

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OR
IF TRAFFIC CONTROL DEVICES
ENCROACH ONTO TRAVELED WAY, USE

POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET	SHOULDER TAPER L / 3 W, LATERAL OFFSET (FT)						BUFFER SPACE (B) FEET
		3	4	5	6	7	8	
25	200'	10	14	17	21	24	28	55
30	200'	15	20	25	30	35	40	85
35	350'	20	27	34	40	47	54	120
40	350'	26	35	44	53	62	70	170
45	500'	45	59	74	89	104	119	220
50	500'	50	66	83	99	116	132	280
55	500'	54	73	91	109	127	145	335'

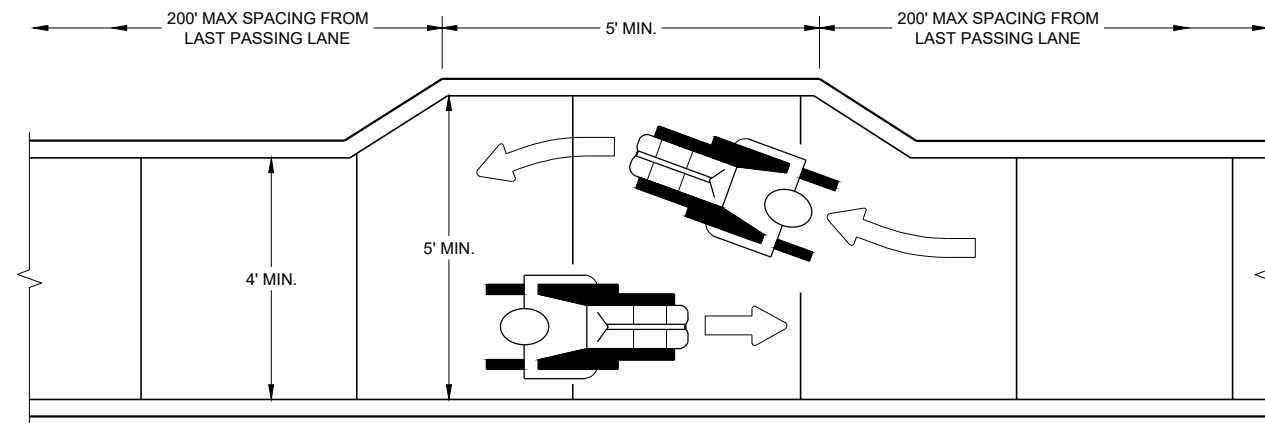
**TRAFFIC CONTROL, WORK ON
SHOULDER OR PARKING LANE,
UNDIVIDED ROADWAY**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

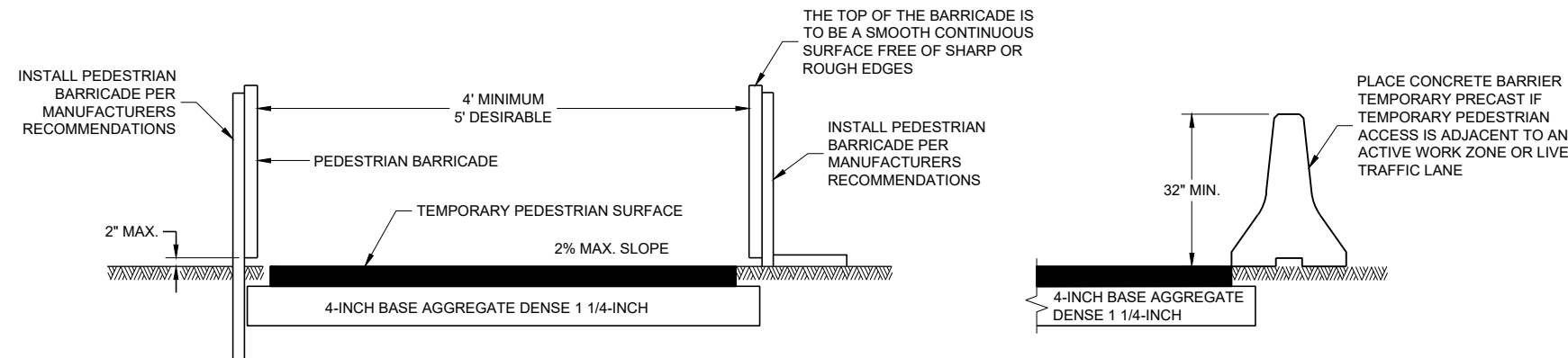
APPROVED
May 2020 /S/ Andrew Heidtke
DATE STATEWIDE WORK ZONE TRAFFIC
SAFETY ENGINEER

SDD 15D28 - 04

SDD 15D28 - 04



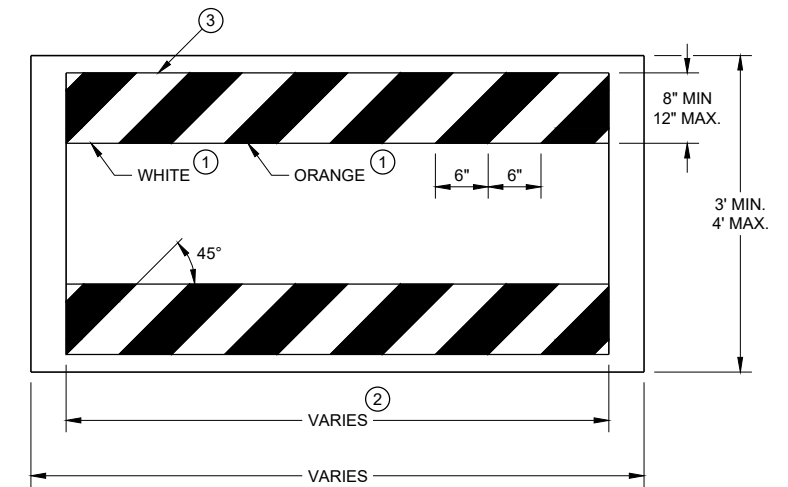
NARROW SIDEWALK PASSING DETAIL



TEMPORARY PEDESTRIAN ACCESS

GENERAL NOTES

- BARRICADE DEVICE SELECTED FROM APPROVED PRODUCT LIST
- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② SHEETING REQUIRED ON MORE THAN 50% OF BARRICADE WIDTH.
- ③ PLACE SHEETING ON BOTH SIDES OF THE BARRICADE.
- * USE THIS DETAIL FOR SHEETING PLACEMENT REFERENCE.

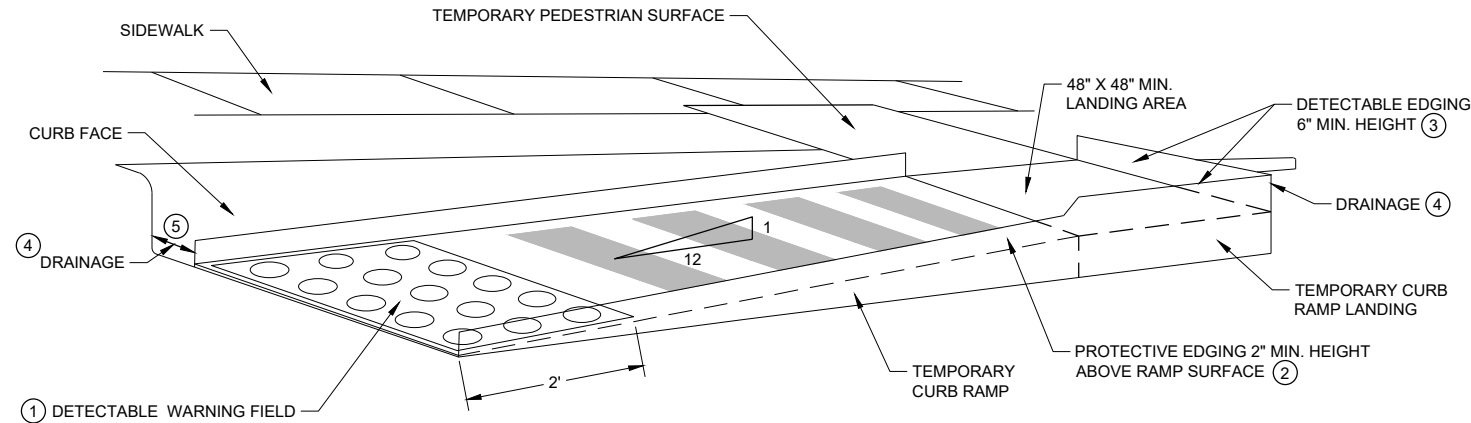


TEMPORARY PEDESTRIAN BARRICADE*

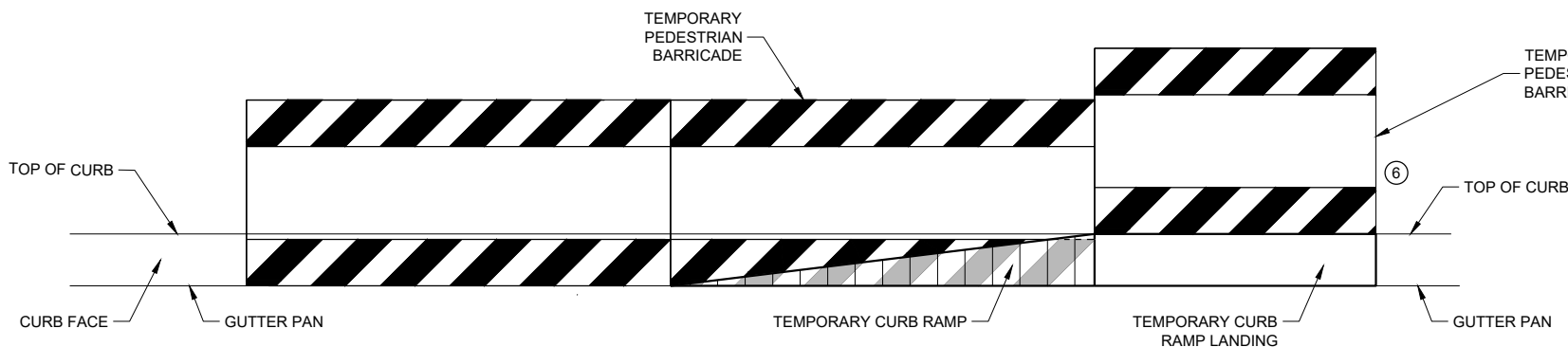
GENERAL NOTES

CURB RAMPS SHALL BE 48" MIN. WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE.
 CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.
 CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.
 LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.
 CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES MAY BE VERTICAL UP TO 1/4" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".

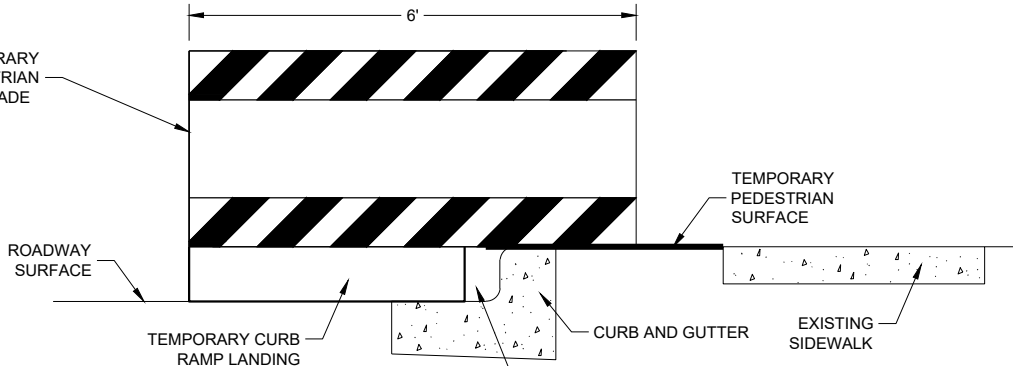
- ① INSTALL CONTRASTING TEMPORARY DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS, AS SHOWN IN THE PLANS.
- ② PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- ③ DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- ④ DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ⑤ ENSURE CURB RAMP IS OUT OF THE GUTTER PAN.
- ⑥ IF ONLY PART OF THE END PANEL OF TEMPORARY PEDESTRIAN BARRICADE PANEL IS NEEDED, EXTEND EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL HERE.



PERSPECTIVE VIEW



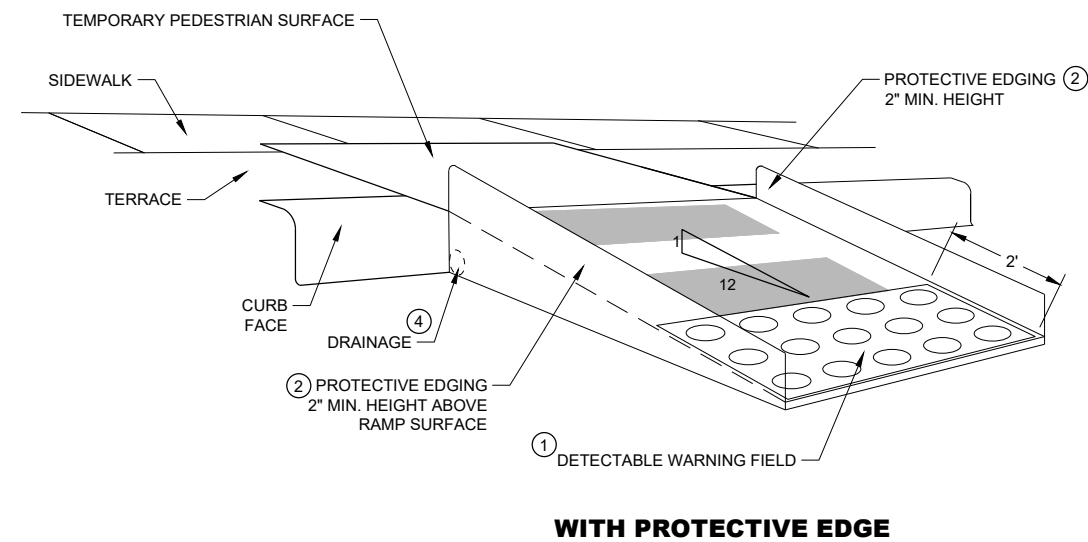
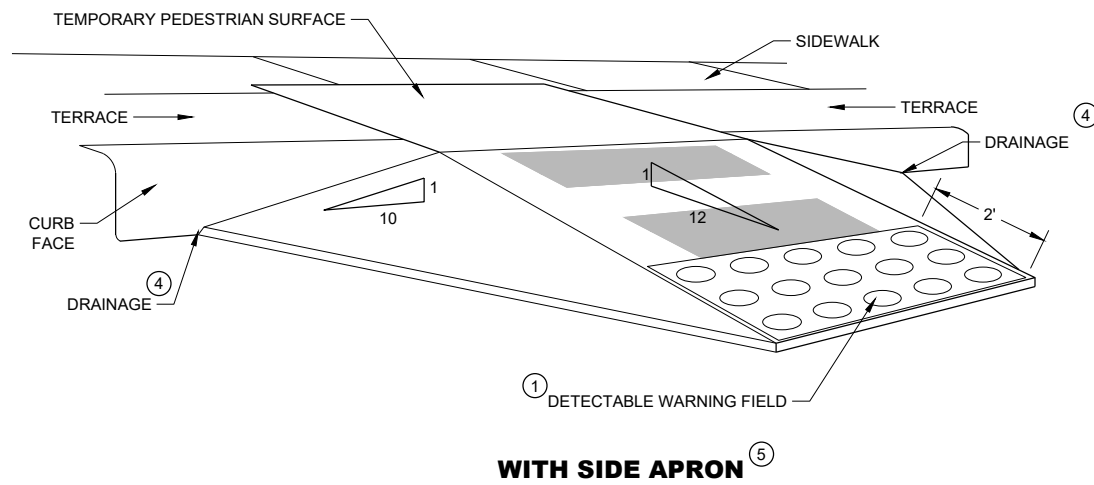
FRONT VIEW



SIDE VIEW

TEMPORARY CURB RAMP PARALLEL TO CURB

<p>TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION</p>
<p>STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION</p>



TEMPORARY CURB RAMP PERPENDICULAR TO CURB

GENERAL NOTES

CURB RAMPS SHALL BE 48" MINIMUM WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE.

ALTERNATE SIDEWALK WORK BETWEEN LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.

CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.

CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.

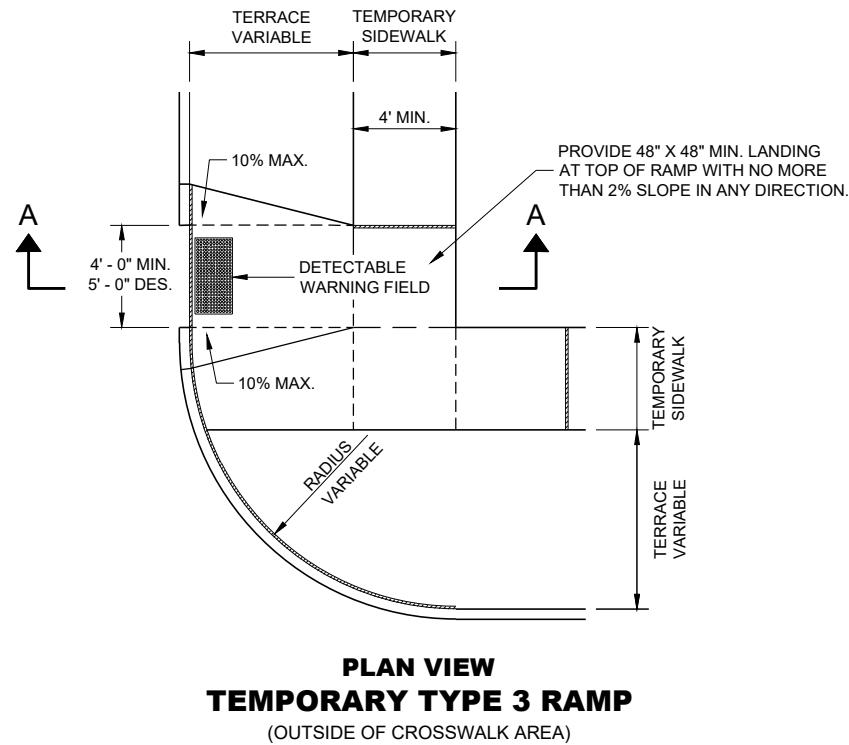
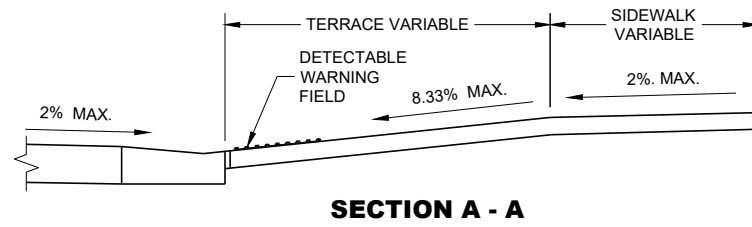
LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.

CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES MAY BE VERTICAL UP TO 1/4" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".

- ① INSTALL CONTRASTING TEMPORARY DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS, AS SHOWN IN THE PLANS
- ② PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- ③ DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- ④ DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ⑤ CAN ONLY BE USED FOR RAMPS WITH 6" OR LESS OF VERTICAL CHANGE.

GENERAL NOTES

- BARRICADE DEVICE SELECTED FROM APPROVED PRODUCT LIST
- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② SHEETING REQUIRED ON MORE THAN 50% OF BARRICADE WIDTH.
- ③ PLACE SHEETING ON BOTH SIDES OF THE BARRICADE.
- ★ USE THIS DETAIL FOR SHEETING PLACEMENT REFERENCE.



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SDD 15D30-09d

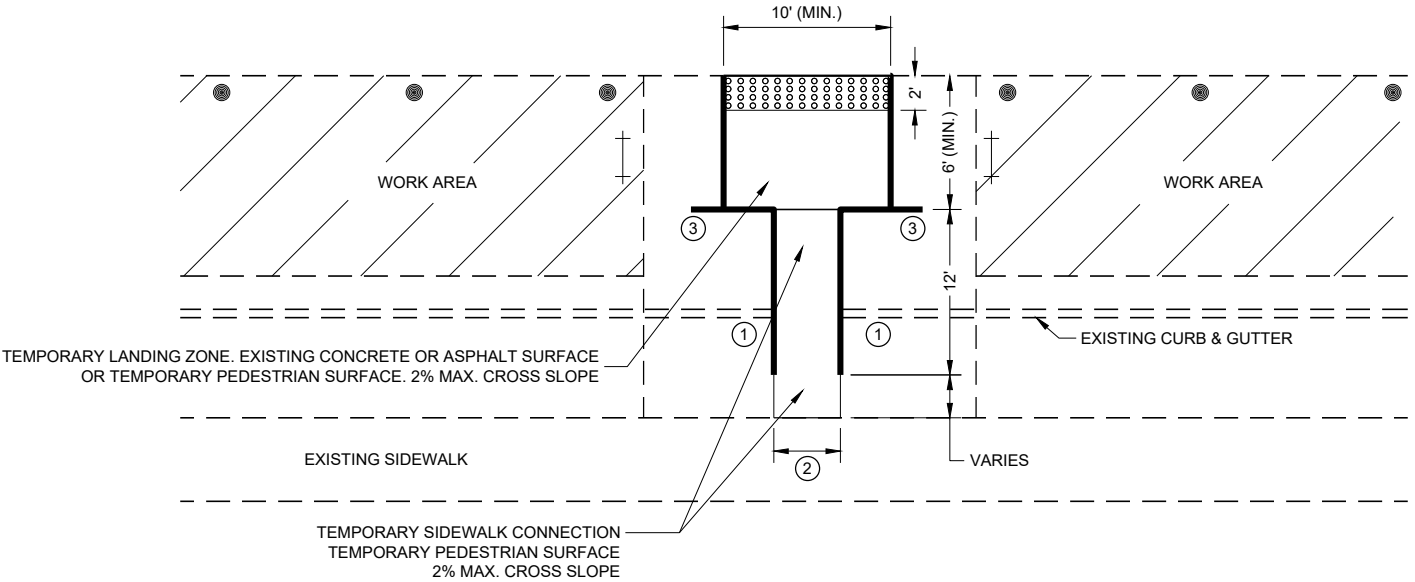
SDD 15D30-09d

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2023 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

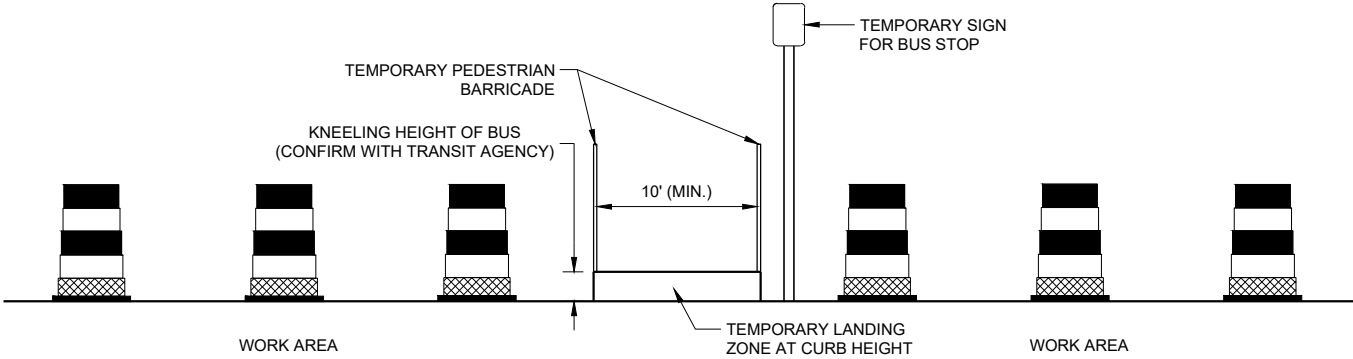
GENERAL NOTES

- TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.
- NOTIFY THE BUS COMPANY 7 DAYS IN ADVANCE OF THE BUS STOP RELOCATION.
- PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMP OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.
- CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES MAY BE VERTICAL UP TO 1/4" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".
- CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.

- ① DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ② 5' WIDE MIN. WITH TEMPORARY PEDESTRIAN BARRICADE, 10' WIDE MIN. WITHOUT TEMPORARY PEDESTRIAN BARRICADE.
- ③ PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE INTO THIS SPACE.



PLAN VIEW



**PROFILE VIEW
TEMPORARY BUS STOP PAD**







LEGEND

- TRAFFIC CONTROL DRUM
- TYPE III BARRICADE
- TEMPORARY PEDESTRIAN BARRICADE
- TEMPORARY DETECTABLE WARNING FIELD
- WORK AREA

**TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION**

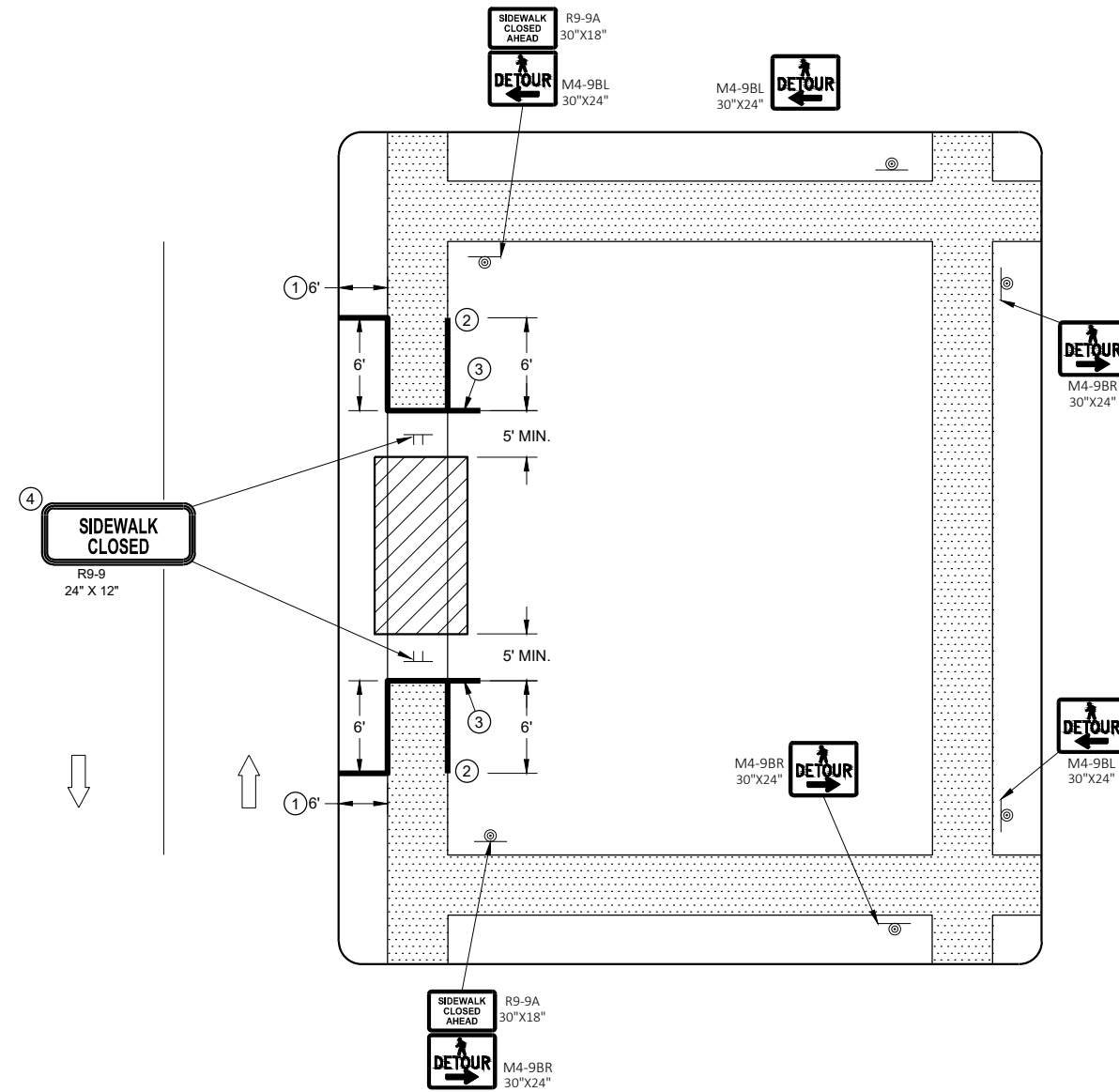
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  SIGN ON TEMPORARY SUPPORT
-  UNDER PEDESTRIAN TRAFFIC
-  WORK AREA
-  TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC

GENERAL NOTES








- SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.
- WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.
- SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.
- PLACE TEMPORARY PEDESTRIAN BARRICADE TO FIT FIELD CONDITIONS, AVOIDING CONFLICTS WITH DRIVEWAYS AND OTHER EXISTING FEATURES.
- ① IF TERRACE IS LESS THAN 6 FEET WIDE, OMIT TEMPORARY PEDESTRIAN BARRICADE FROM THE SIDEWALK TO THE CURB.
 - ② PLACE BARRICADE CLOSURE SO THAT THE TEMPORARY PEDESTRIAN BARRICADE END IS AT THE LAST OPEN SIDEWALK ACCESS TO RESIDENCES OR BUSINESSES BEFORE THE SIDEWALK CLOSURE.
 - ③ IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
 - ④ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.



SIDEWALK DETOUR, SIDEWALK ONLY ON ONE SIDE

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  WORK AREA
-  UNDER PEDESTRIAN TRAFFIC
-  TEMPORARY PEDESTRIAN SURFACE
-  TEMPORARY PEDESTRIAN BARRICADE
-  OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC

GENERAL NOTES

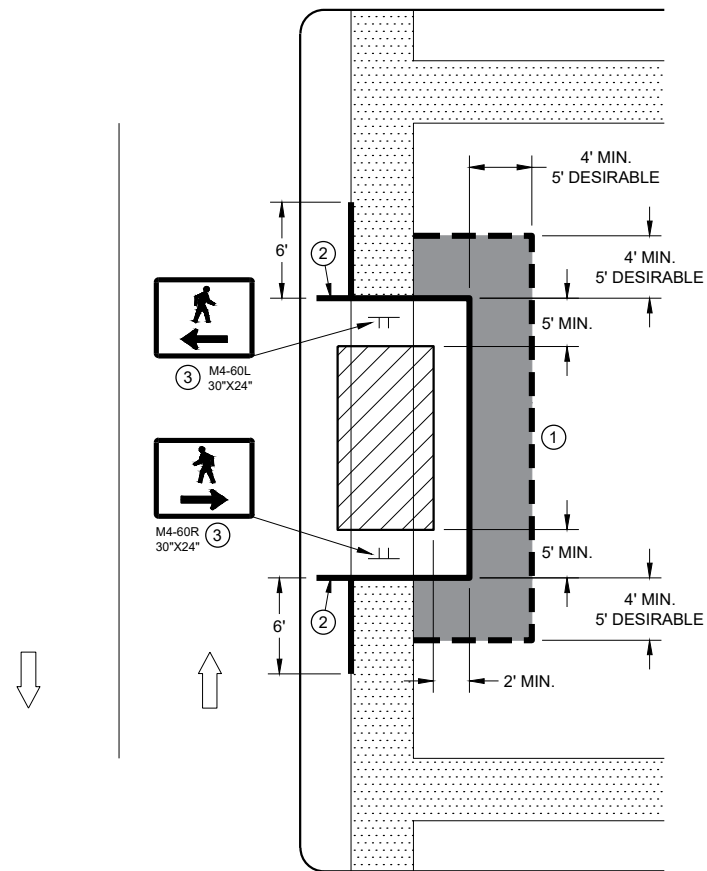
- TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.
- SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.
- WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.
- SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.
- ① USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
- ② IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
- ③ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.

6

6

SDD 15D30 - 09g










SDD 15D30 - 09g



**SIDEWALK DIVERSION
SINGLE SIDE**

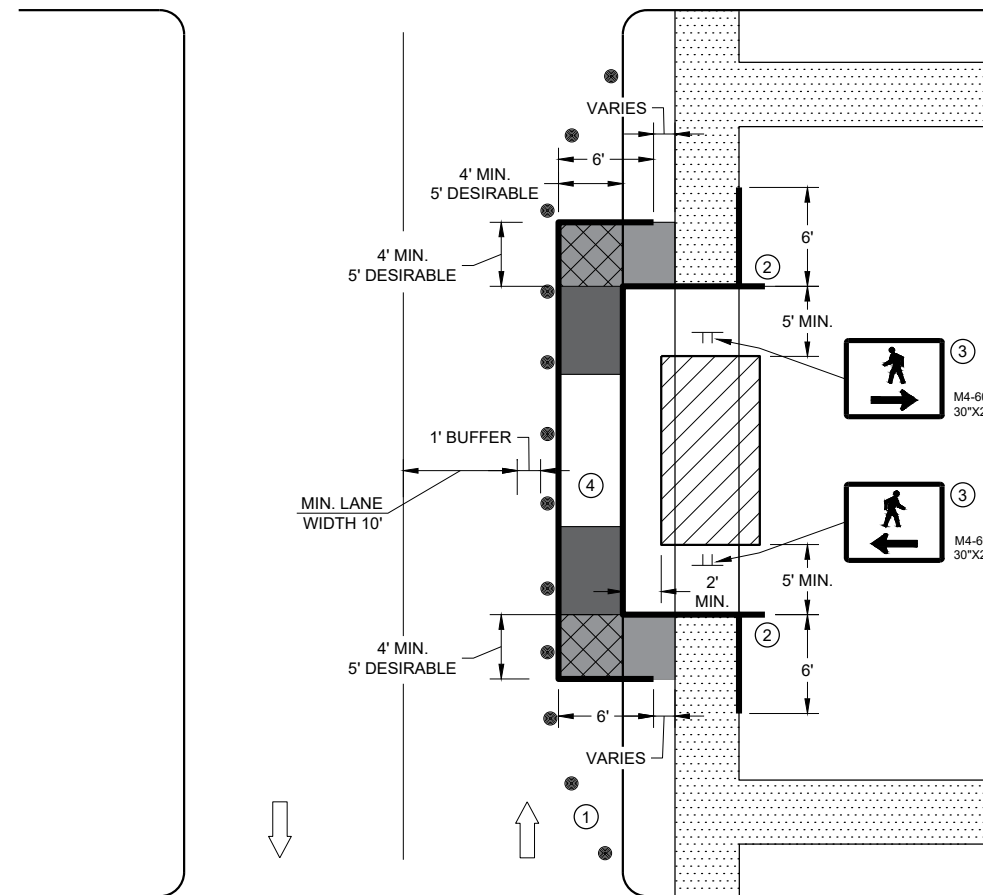
TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL DRUM
-  WORK AREA
-  UNDER PEDESTRIAN TRAFFIC
-  TEMPORARY CURB RAMP
-  TEMPORARY PEDESTRIAN SURFACE "A"
-  TEMPORARY PEDESTRIAN SURFACE "B"
-  TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC

GENERAL NOTES

- TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.
- SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.
- WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.
- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND BUFFER SPACE REQUIRED.
 - ② PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL PAST THE SIDEWALK ON THE SIDE AWAY FROM THE ROAD.
 - ③ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.
 - ④ USE EXISTING PAVEMENT SURFACE. IF EXISTING PAVEMENT SURFACE HAS BEEN REMOVED, USE A TEMPORARY PEDESTRIAN SURFACE.



SIDEWALK DIVERSION, SINGLE SIDE

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

IF PEDESTRIAN PUSH BUTTONS ARE PRESENT ON THE EXISTING FACILITY, ENSURE THEY ARE MAINTAINED/ACCESSIBLE FOR PEDESTRIAN USE THROUGHOUT THE TEMPORARY PEDESTRIAN ACCOMMODATIONS.











SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

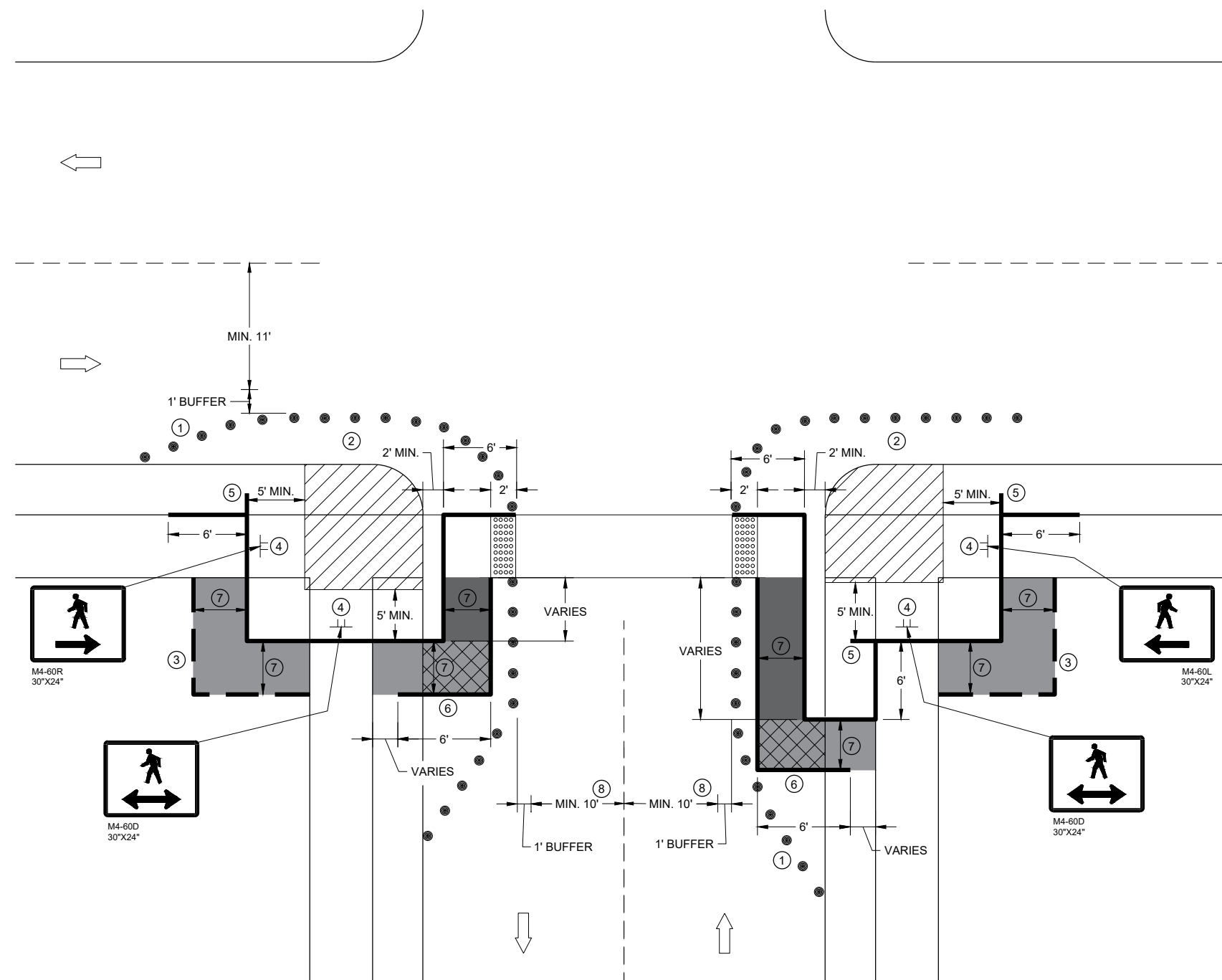
TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG

WHEN TEMPORARY PEDESTRIAN BARRICADE RUNS PARALLEL ALONG THE SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND PROPER BUFFER SPACE REQUIRED.
- ② PROVIDE ADEQUATE SPACE FOR CONTRACTOR OPERATIONS
- ③ USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
- ④ MOUNTING HEIGHT OF 5 FEET FROM SIDEWALK SURFACE TO BOTTOM OF SIGN.
- ⑤ PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL IN THE SIDEWALK TERRACE.
- ⑥ IF TEMPORARY PEDESTRIAN BARRICADE DOES NOT REACH THE FACE OF THE CURB, USE AN ADDITIONAL PANEL AND EXTEND INTO THE TERRACE.
- ⑦ 4 FEET MINIMUM, 5 FEET DESIRABLE
- ⑧ IF MINIMUM LANE WIDTHS CAN'T BE ATTAINED, CURB RAMPS MAY NEED TO BE CONSTRUCTED AT SEPARATE TIMES.

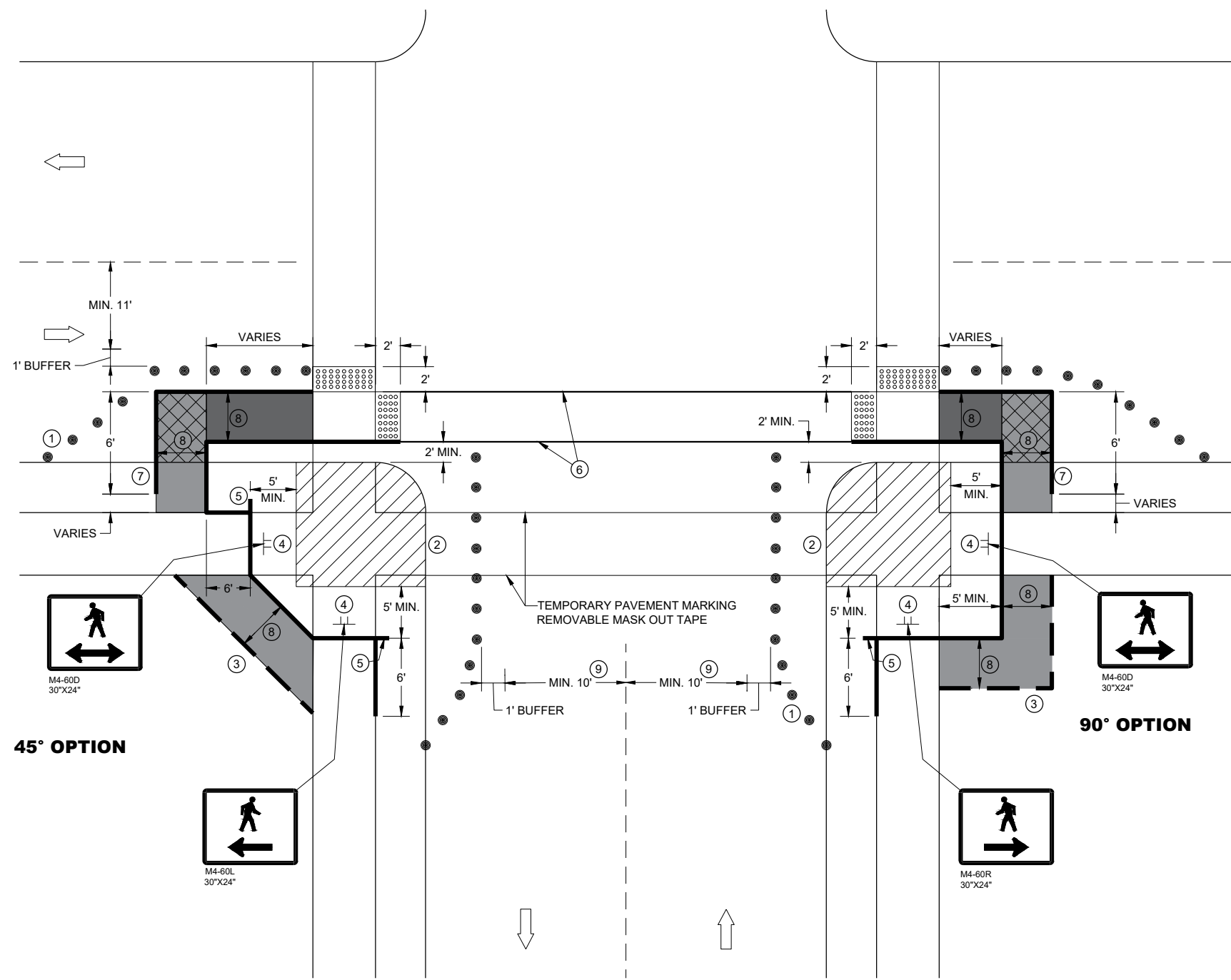
LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL DRUM
-  WORK AREA
-  TEMPORARY CURB RAMP
-  TEMPORARY PEDESTRIAN SURFACE "A"
-  TEMPORARY PEDESTRIAN SURFACE "B"
-  TEMPORARY DETECTABLE WARNING FIELD
-  TEMPORARY PEDESTRIAN BARRICADE
-  OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC



**CURB RAMP PEDESTRIAN TRAFFIC CONTROL
SIDEWALK ON SINGLE SIDE**

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION



CURB RAMP PEDESTRIAN TRAFFIC CONTROL

GENERAL NOTES

IF PEDESTRIAN PUSH BUTTONS ARE PRESENT ON THE EXISTING FACILITY, ENSURE THEY ARE MAINTAINED/ACCESSIBLE FOR PEDESTRIAN USE THROUGHOUT THE TEMPORARY PEDESTRIAN ACCOMMODATIONS.

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG

WHEN TEMPORARY PEDESTRIAN BARRICADE RUNS PARALLEL ALONG THE SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND PROPER BUFFER SPACE REQUIRED.
- ② PROVIDE ADEQUATE SPACE FOR CONTRACTOR OPERATIONS
- ③ USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
- ④ MOUNTING HEIGHT OF 5 FEET FROM SIDEWALK SURFACE TO BOTTOM OF SIGN.
- ⑤ PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL IN THE SIDEWALK TERRACE.
- ⑥ WHITE 6" TEMPORARY PAVEMENT MARKING
- ⑦ IF TEMPORARY PEDESTRIAN BARRICADE DOES NOT REACH THE FACE OF THE CURB, USE AN ADDITIONAL PANEL AND EXTEND INTO THE TERRACE.
- ⑧ 4 FEET MINIMUM, 5 FEET DESIRABLE
- ⑨ IF MINIMUM LANE WIDTHS CAN'T BE ATTAINED, CURB RAMPS MAY NEED TO BE CONSTRUCTED AT SEPARATE TIMES.







LEGEND

- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL DRUM
- WORK AREA
- TEMPORARY CURB RAMP
- TEMPORARY PEDESTRIAN SURFACE "A"
- TEMPORARY PEDESTRIAN SURFACE "B"
- TEMPORARY DETECTABLE WARNING FIELD
- TEMPORARY PEDESTRIAN BARRICADE
- OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
- DIRECTION OF TRAFFIC

**TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION**

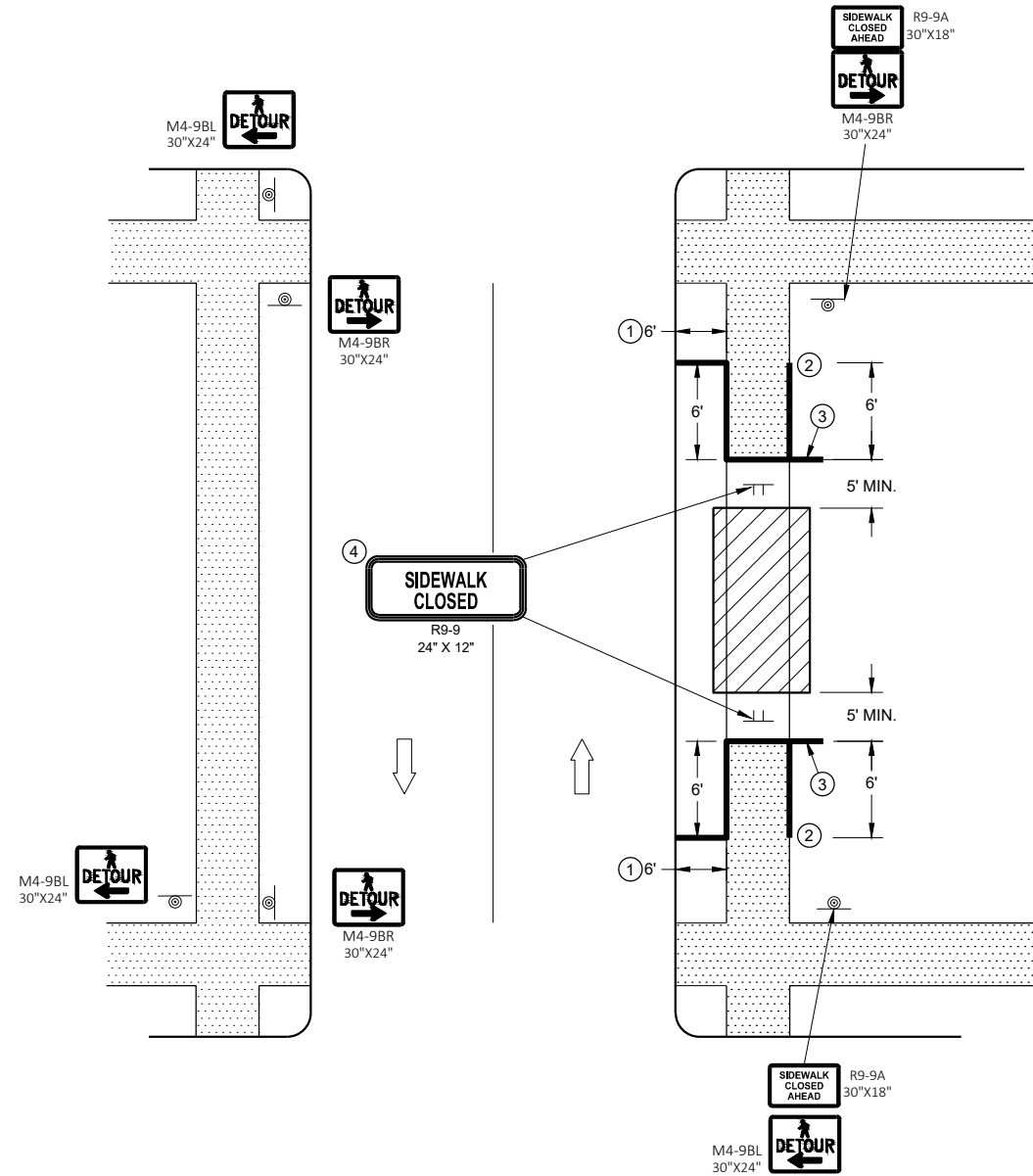
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  SIGN ON TEMPORARY SUPPORT
-  UNDER PEDESTRIAN TRAFFIC
-  WORK AREA
-  TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC

GENERAL NOTES

- SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.
- WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.
- SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.
- PLACE TEMPORARY PEDESTRIAN BARRICADE TO FIT FIELD CONDITIONS, AVOIDING CONFLICT WITH DRIVEWAYS AND OTHER EXISTING FEATURES.
- ① IF TERRACE IS LESS THAN 6 FEET WIDE, OMIT TEMPORARY PEDESTRIAN BARRICADE FROM THE SIDEWALK TO THE CURB.
 - ② PLACE BARRICADE CLOSURE SO THAT THE TEMPORARY PEDESTRIAN BARRICADE END IS AT THE LAST OPEN SIDEWALK ACCESS TO RESIDENCES OR BUSINESSES BEFORE THE SIDEWALK CLOSURE.
 - ③ IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
 - ④ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.



SIDEWALK DETOUR, SIDEWALK ON BOTH SIDES

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

GENERAL NOTES



TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.

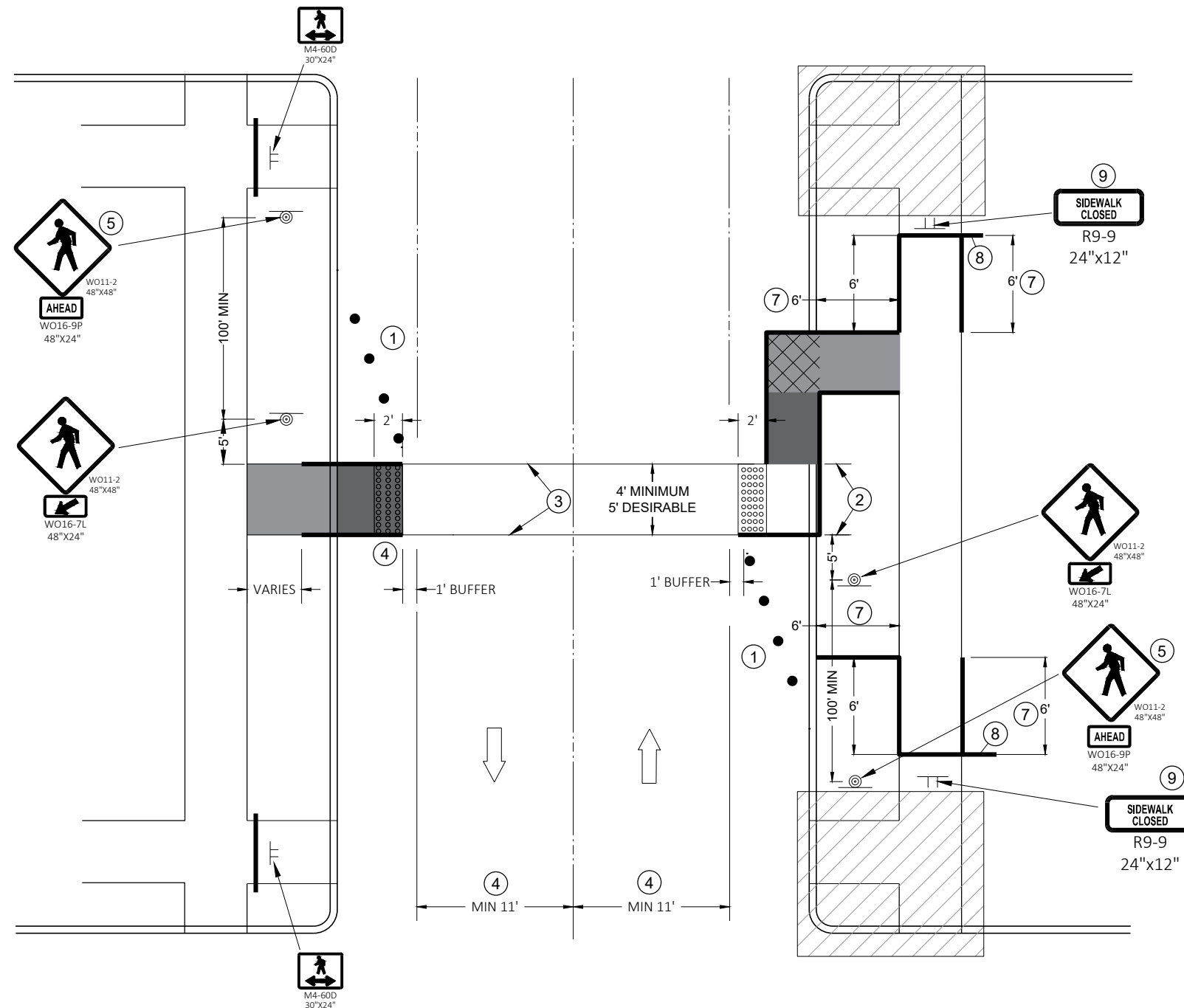
SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

SEE OTHER PEDESTRIAN ACCOMMODATION DETAILS FOR SIGNING AND DEVICES FOR DIFFERENT PEDESTRIAN FACILITIES CLOSURES.

- ① SHOULDER OR LANE CLOSURE ADVANCED WARNING AND PROPER BUFFER SPACE REQUIRED.
- ② 4 FEET MINIMUM, 5 FEET DESIRABLE.
- ③ WHITE 6" TEMPORARY PAVEMENT MARKING.
- ④ IF MINIMUM LANE WIDTHS CAN'T BE ATTAINED, PERPENDICULAR CURB RAMPS MAY NEED TO BE UTILIZED.
- ⑤ IF MINIMUM 100' SPACING FROM THE MID-BLOCK CROSSING CANNOT BE ATTAINED BEFORE THE INTERSECTION, REMOVE THIS SIGN ASSEMBLY.
- ⑥ IF TERRACE IS LESS THAN 6 FEET WIDE, OMIT TEMPORARY PEDESTRIAN BARRICADE FROM THE SIDEWALK TO THE CURB.
- ⑦ PLACE BARRICADE CLOSURE SO THAT THE TEMPORARY PEDESTRIAN BARRICADE END IS AT THE LAST OPEN SIDEWALK ACCESS TO RESIDENCES OR BUSINESSES BEFORE THE SIDEWALK CLOSURE.
- ⑧ IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF THE EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
- ⑨ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF THE SIGN.

LEGEND

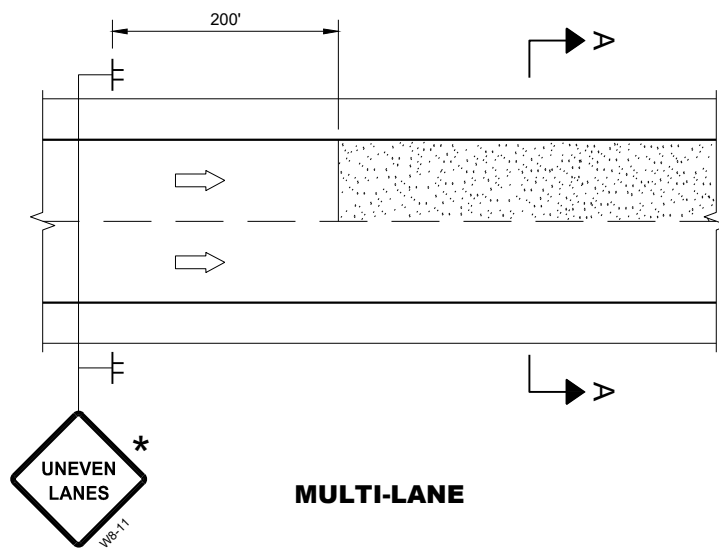
	TRAFFIC CONTROL DRUM
	SIGN ON TEMPORARY SUPPORT
	TEMPORARY CURB RAMP
	TEMPORARY DETECTABLE WARNING FIELD
	TEMPORARY PEDESTRIAN SURFACE "A"
	TEMPORARY PEDESTRIAN SURFACE "B"
	WORK AREA
	TEMPORARY PEDESTRIAN BARRICADE
	DIRECTION OF TRAFFIC



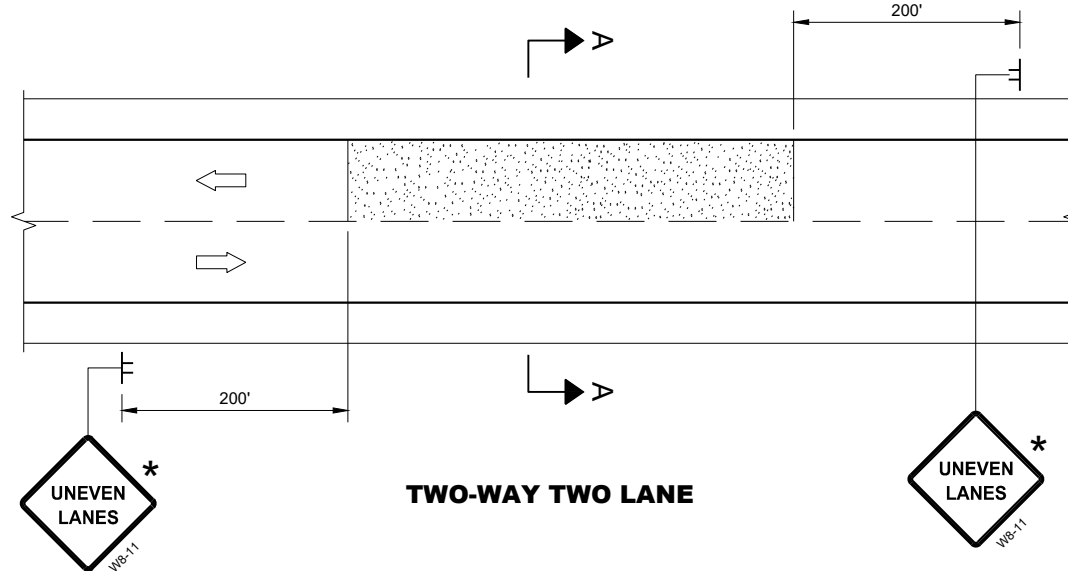
TEMPORARY PEDESTRIAN CROSSING

**TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION**

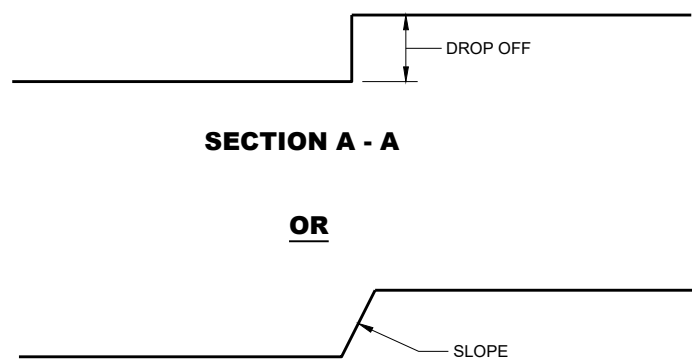
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



MULTI-LANE



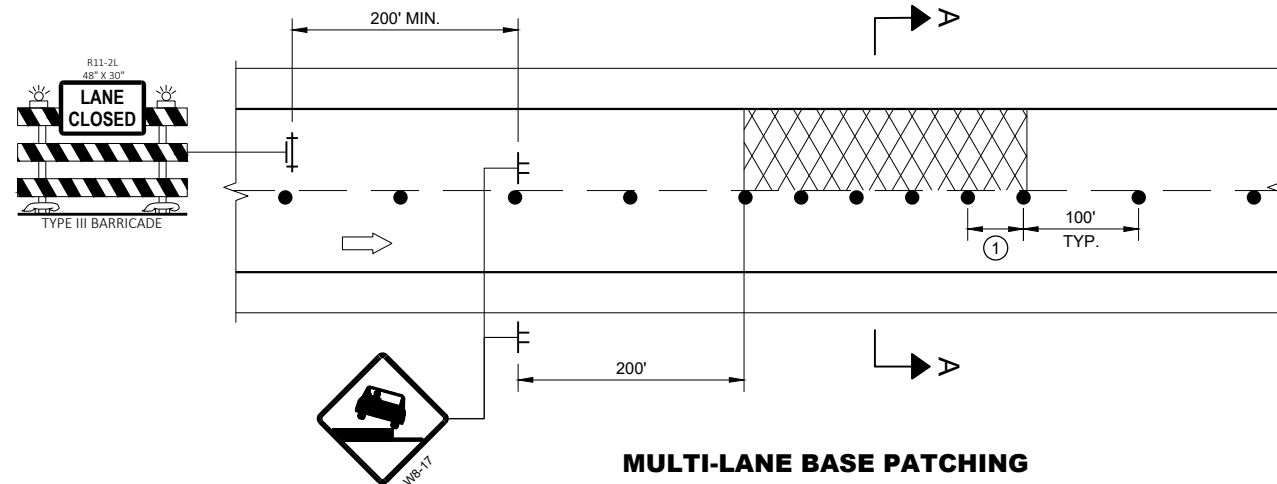
TWO-WAY TWO LANE



SECTION A - A

OR

SECTION A - A



MULTI-LANE BASE PATCHING

ADJACENT LANE DROP-OFFS

GENERAL NOTES

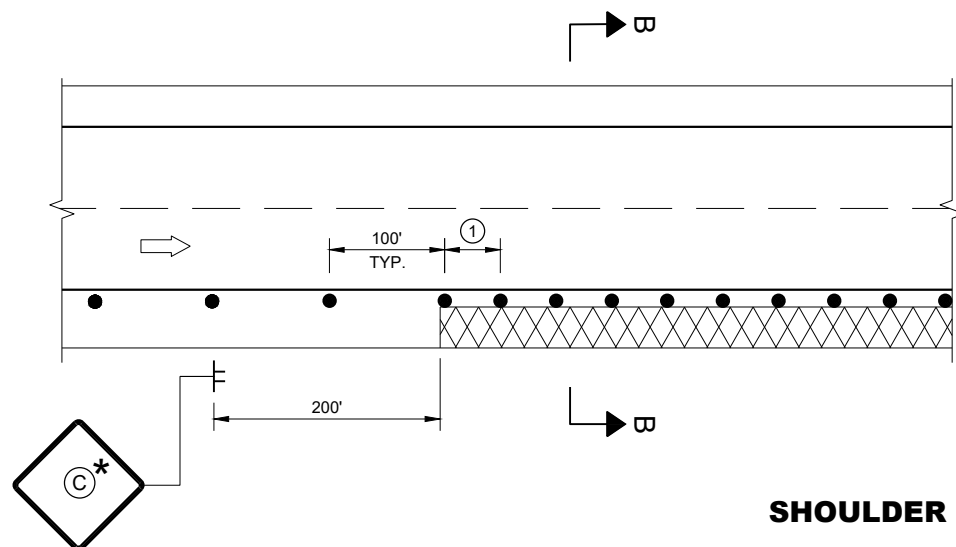
- FOR SPOT LOCATIONS USE ENGINEERING JUDGEMENT WHEN PLACING ADDITIONAL SIGNS.
- ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.
- "WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.
- WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.
- * IF THE DROP-OFF IS CONTINUOUS ALONG THE PROJECT, PLACE ADDITIONAL SIGNS EVERY 1 MILE AND AFTER EVERY ENTRANCE RAMP.
- ① USE CLOSER SPACING WHEN DELINEATING DROP-OFF.

LEGEND

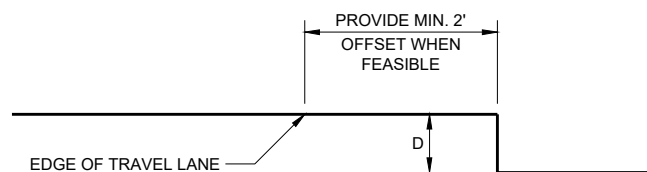
- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL DRUM
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- DIRECTION OF TRAFFIC
- WORK AREA WITH DROP-OFF
- MILLED SURFACE

6

6



SHOULDER DROP-OFFS



SECTION B - B

D	SIGN (C)
< 2" WITH A SLOPE STEEPER THAN 3:1	LOW SHOULDER WO8-9
2" < 6" WITH A SLOPE STEEPER THAN 3:1	SHOULDER DROP - OFF W8-9A PROVIDE A 3:1 OR FLATTER SLOPE OF MATERIAL ADJACENT TO THE PAVEMENT

SDD 15D39 - 02

SDD 15D39 - 02

**TRAFFIC CONTROL,
DROP-OFF SIGNING**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
March 2018 /S/ Andrew Heidtke
DATE DATE WORK ZONE ENGINEER

FHWA

GENERAL NOTES

DRAWING NOT TO SCALE. ALL SIGNS AND POSTS ON THIS SHEET SHALL BE PAID FOR WITH 'TRAFFIC CONTROL SIGNS' BID ITEM. ALL SIDE ROADS WHICH ARE UNDER CONSTRUCTION OF CURB AND GUTTER AND/OR GRADING SHALL BE ADEQUATELY SIGNED.

ALL SIGNS AND DEVICES SHALL BE IN CONFORMANCE WITH THE WISCONSIN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD). SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISDOT STANDARD SIGN PLATES.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THAT THE BACKGROUND IS ORANGE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

ALL SIGNS INAPPROPRIATE TO THE STATUS OF THE CONTROL ZONE, INCLUDING PRE-EXISTING SIGNS IN THE VICINITY, SHALL BE COVERED OR REMOVED AS DIRECTED BY THE ENGINEER.

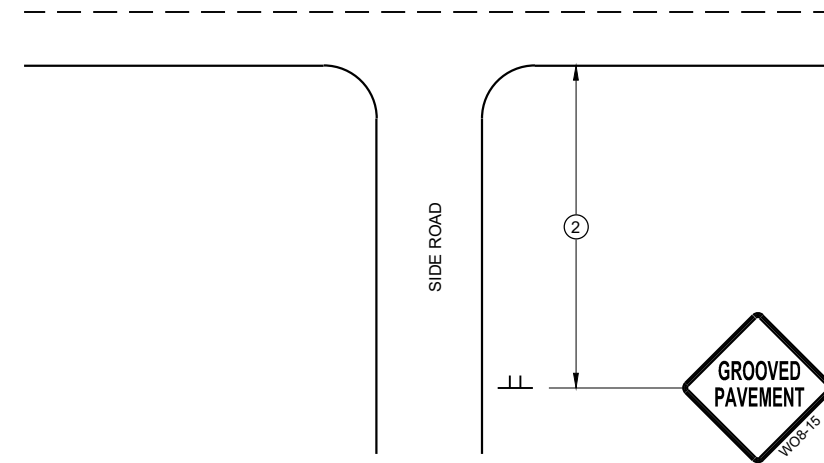
SEE 15C34 FOR ADDITIONAL TRAFFIC CONTROL SIGNING WHEN CENTERLINE PAVEMENT MAKINGS ARE MISSING. 'DO NOT PASS' SIGNS MUST BE INSTALLED ON THE SAME DAY AS MILLING OPERATIONS.

- ① PLACE SIGNS 350' IN ADVANCE OF MILLED SURFACES AND AT 1 MILE INTERVALS, OR AS DIRECTED BY THE ENGINEER.
- ② PLACE SIGN 200' MIN. FROM INTERSECTION AND 200' MIN. AFTER ADVANCE WARNING SIGN SHOWN IN SDD 15C04.

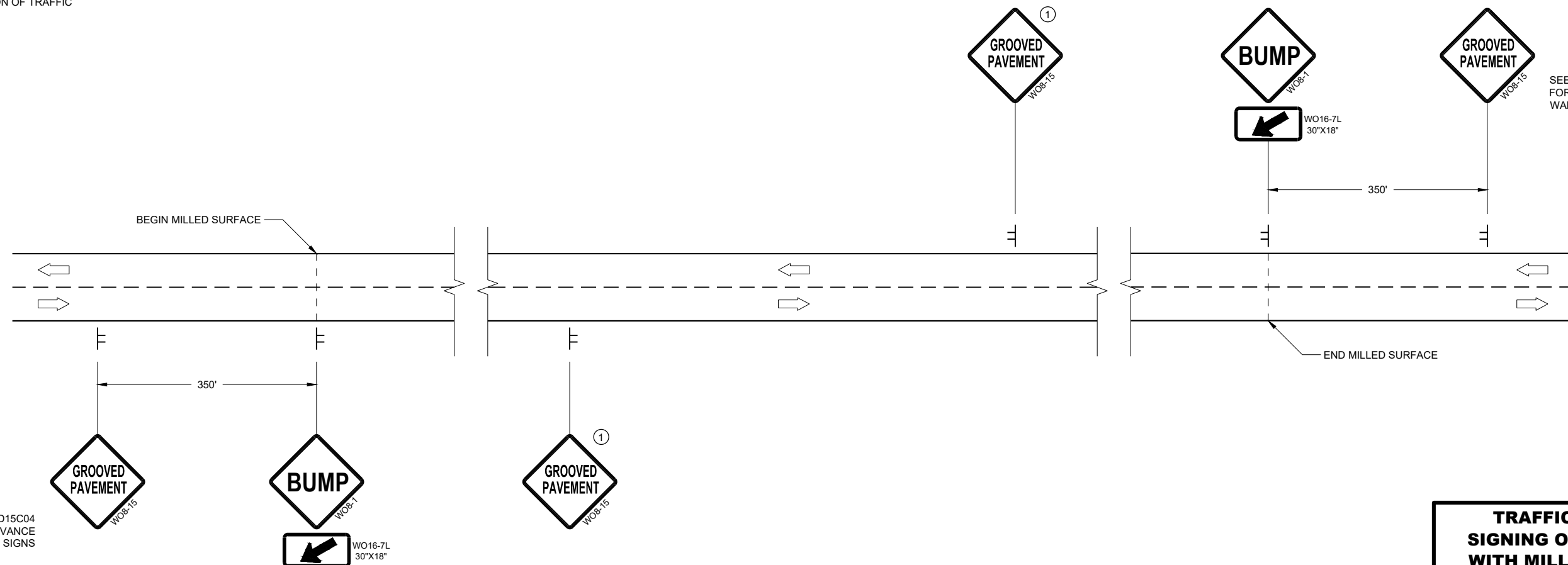
LEGEND

⊥ SIGN ON TEMPORARY SUPPORT

⇨ DIRECTION OF TRAFFIC



TYPICAL SIDE ROAD APPROACH SIGN DETAIL



SEE SDD15C04 FOR ADVANCE WARNING SIGNS

SEE SDD15C04 FOR ADVANCE WARNING SIGNS

DETAIL FOR SIGNING ON MILLED SURFACES

TRAFFIC CONTROL, SIGNING ON ROADWAYS WITH MILLED SURFACES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA

GENERAL NOTES

DRAWING NOT TO SCALE. ALL SIGNS AND POSTS ON THIS SHEET SHALL BE PAID FOR WITH 'TRAFFIC CONTROL SIGNS' BID ITEM. ALL SIDE ROADS WHICH ARE UNDER CONSTRUCTION OF CURB AND GUTTER AND/OR GRADING SHALL BE ADEQUATELY SIGNED.

ALL SIGNS AND DEVICES SHALL BE IN CONFORMANCE WITH THE WISCONSIN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD). SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISDOT STANDARD SIGN PLATES.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THAT THE BACKGROUND IS ORANGE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

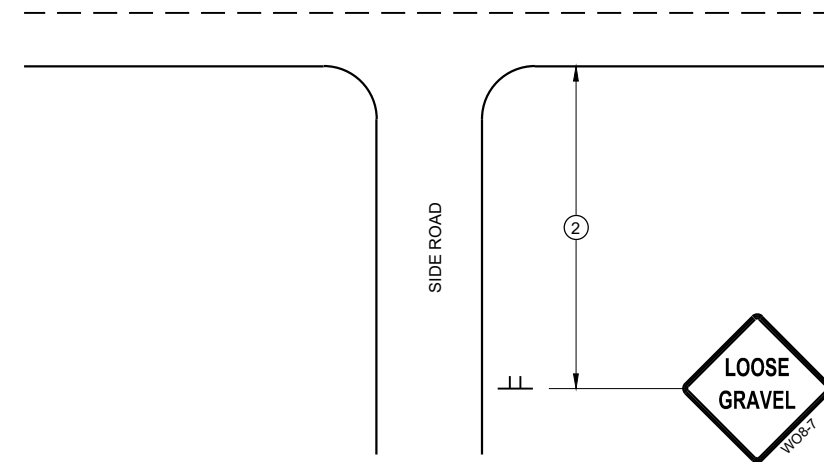
ALL SIGNS INAPPROPRIATE TO THE STATUS OF THE CONTROL ZONE, INCLUDING PRE-EXISTING SIGNS IN THE VICINITY, SHALL BE COVERED OR REMOVED AS DIRECTED BY THE ENGINEER.

SEE 15C34 FOR ADDITIONAL TRAFFIC CONTROL SIGNING WHEN CENTERLINE PAVEMENT MAKINGS ARE MISSING. 'DO NOT PASS' SIGNS MUST BE INSTALLED ON THE SAME DAY AS MILLING OPERATIONS.

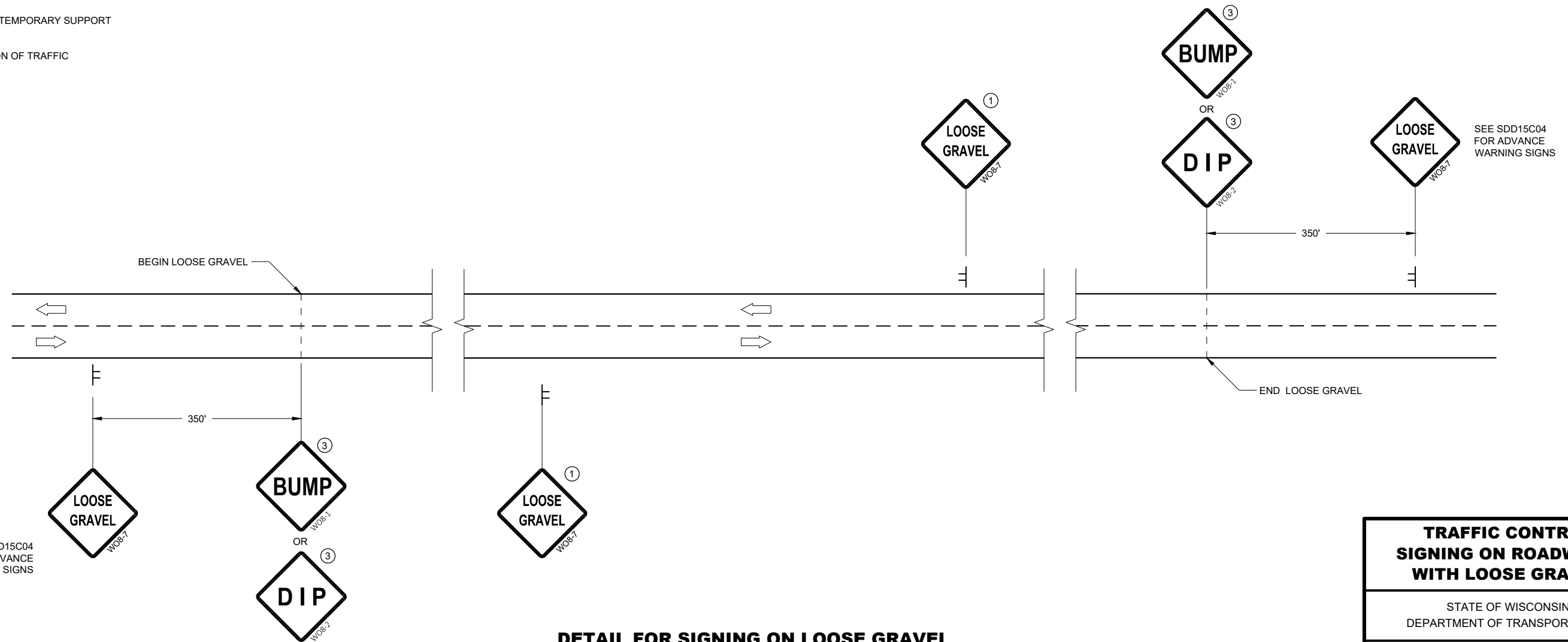
- ① PLACE SIGNS 350' IN ADVANCE OF CHIP SEALED OR LOOSE GRAVEL SURFACES AND AT 1 MILE INTERVALS, OR AS DIRECTED BY THE ENGINEER.
- ② PLACE SIGN 200' MIN. FROM INTERSECTION AND 200' MIN. AFTER ADVANCE WARNING SIGN SHOWN IN SDD 15C04.
- ③ ADD WO8-1 OR WO8-2 SIGN WHEN THE CONDITION IS PRESENT.

LEGEND

- ⊥ SIGN ON TEMPORARY SUPPORT
- ➡ DIRECTION OF TRAFFIC



TYPICAL SIDE ROAD APPROACH SIGN DETAIL

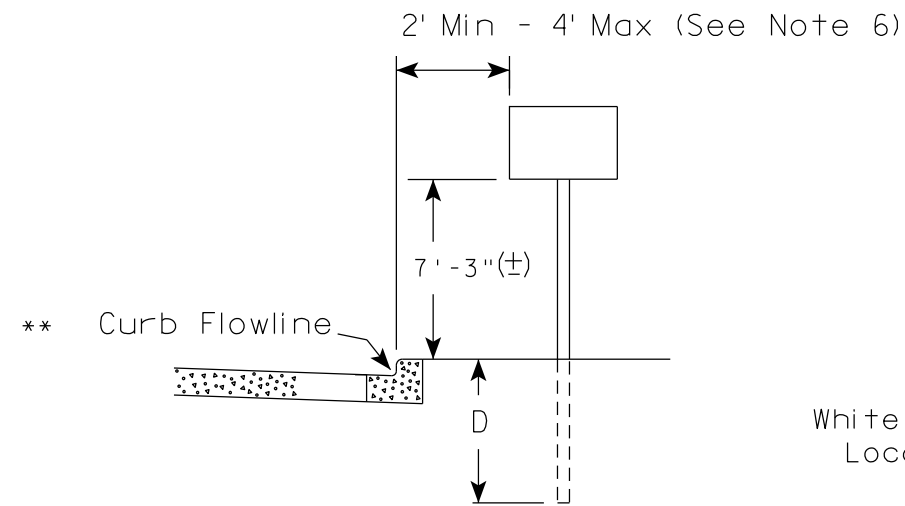


DETAIL FOR SIGNING ON LOOSE GRAVEL OR CHIP SEALED SURFACES

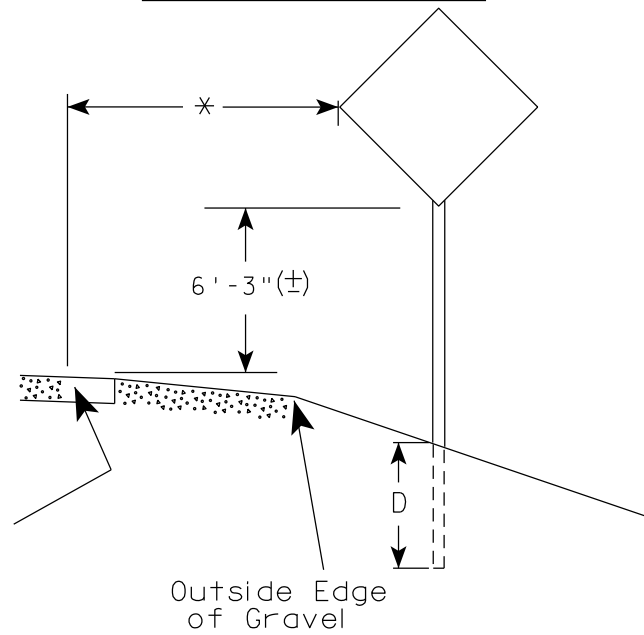
TRAFFIC CONTROL SIGNING ON ROADWAYS WITH LOOSE GRAVEL	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED February 2021 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

URBAN AREA

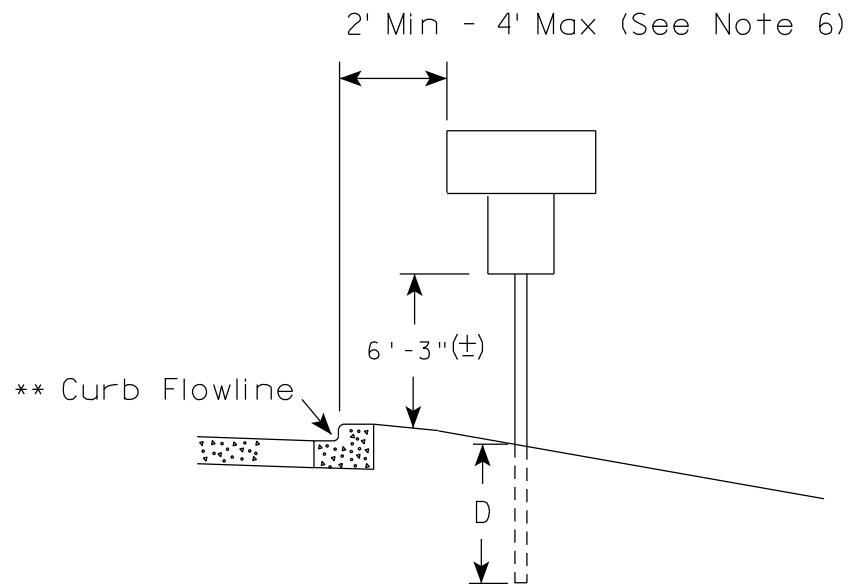
RURAL AREA (See Note 2)



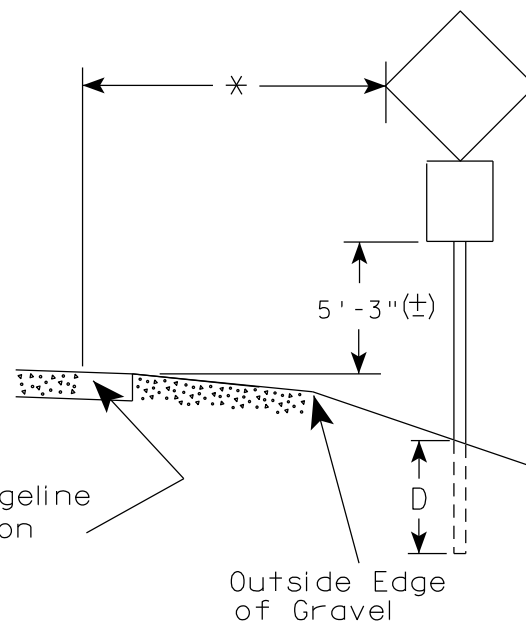
White Edgeline Location



Outside Edge of Gravel



White Edgeline Location



Outside Edge of Gravel

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

GENERAL NOTES

1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on or behind barrier wall, see A4-10 sign plate.
The Double Arrow sign (W12-1D) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).
3. For expressways and freeways, mounting height is 7'- 3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
4. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±).
5. Offset distance shall be consistent with existing signs or consistent throughout length of project.
6. The (±) tolerance for mounting height is 3 inches.
7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the Engineer.

* * The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

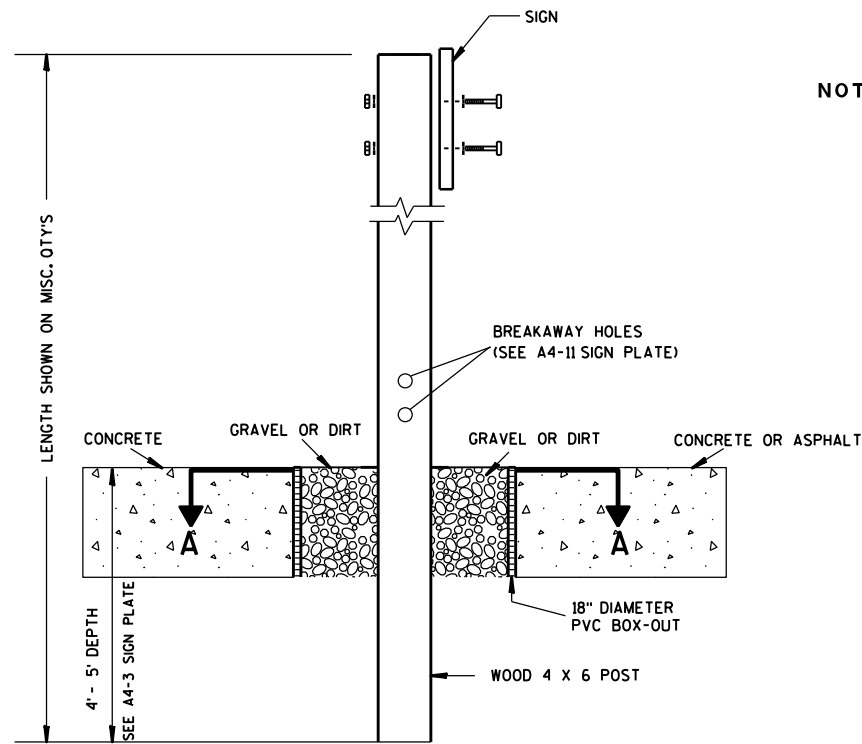
* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

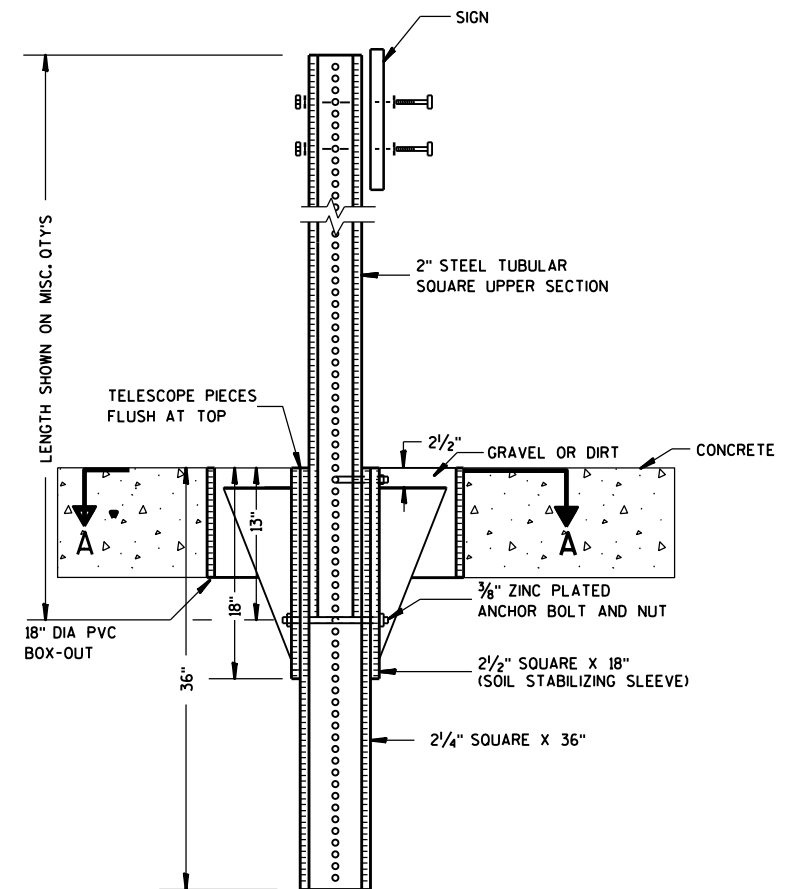
DATE 5/13/2020 PLATE NO. A4-3.22



ELEVATION VIEW

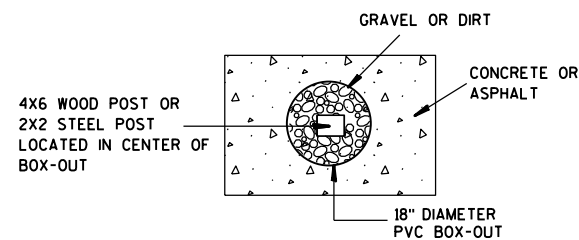
DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

- NOTES:**
1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
 2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
 3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



PLAN VIEW

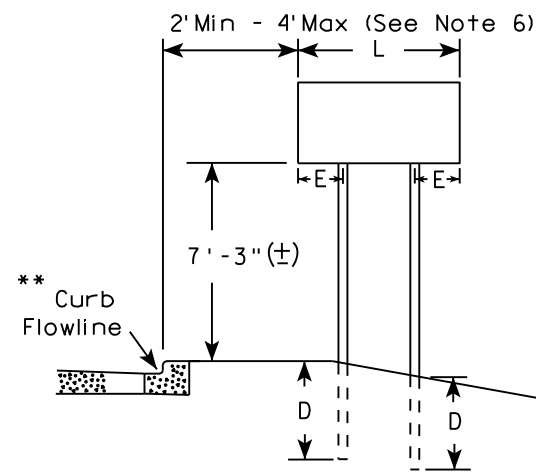
FOR NEW CONCRETE/ ASPHALT INSTALLATIONS

SIGN POST BOX-OUTS A4-3B	
<small>WISCONSIN DEPT OF TRANSPORTATION</small>	
APPROVED <i>Matthew R. Rauch</i> <small>for State Traffic Engineer</small>	
<small>DATE 1/27/14</small>	<small>PLATE NO. A4-3B.1</small>

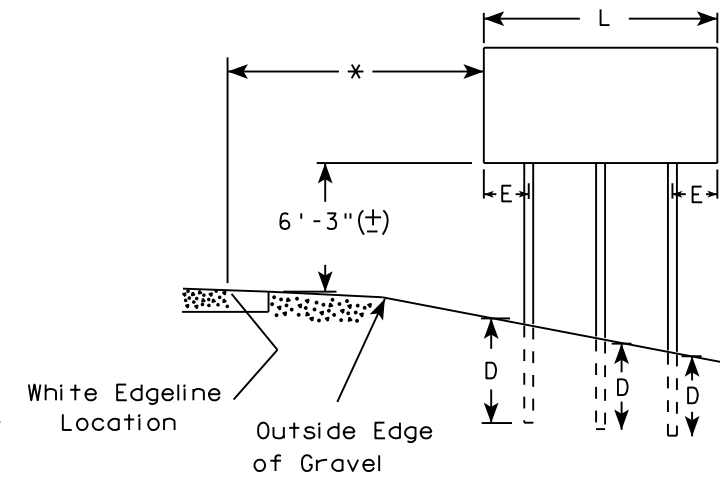
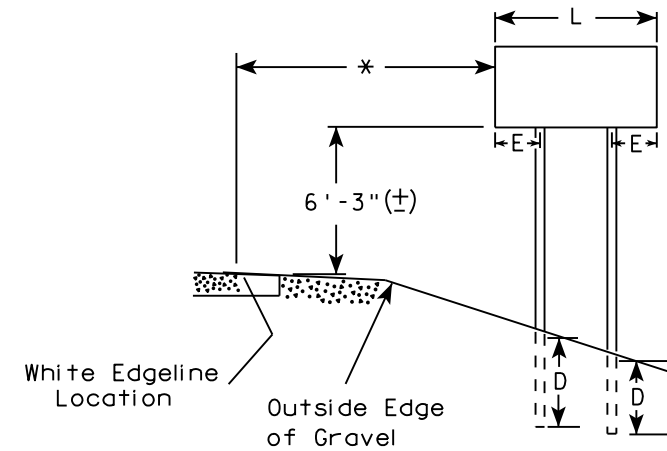
GENERAL NOTES

1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
2. See tables below for required number of posts.
3. For expressways and freeways, mounting height is 7'-3" (±) or 6'-3" (±) depending upon existence of sub-sign.
4. The (±) tolerance for mounting height is 3 inches.
5. J-Assemblies are considered to be one sign for mounting height.
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the engineer.
8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).

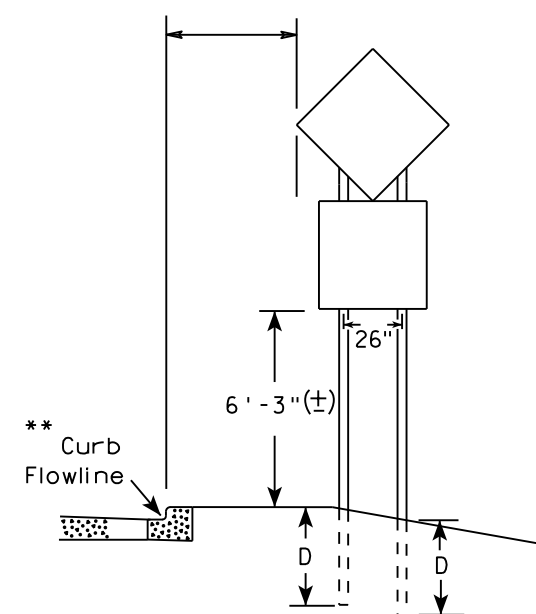
URBAN AREA



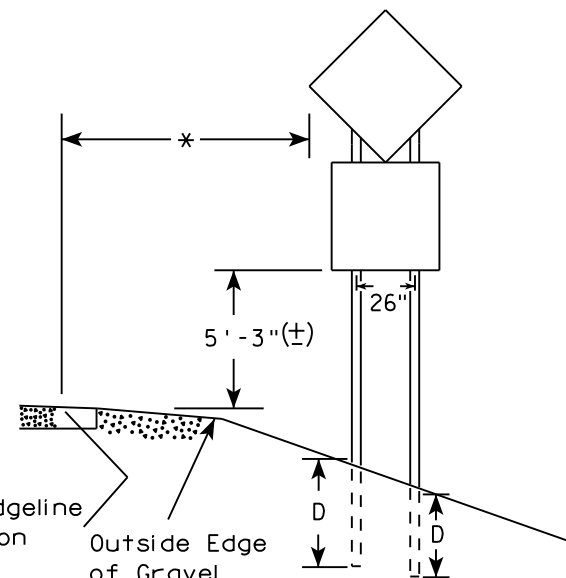
RURAL AREA (See Note 3)



2' Min - 4' Max (See Note 6)



48" DIAMOND WARNING SIGN



48" DIAMOND WARNING SIGN

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

*** See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)	
L	E
Greater than 48" Less than 60"	12"
60" to 108"	L/5

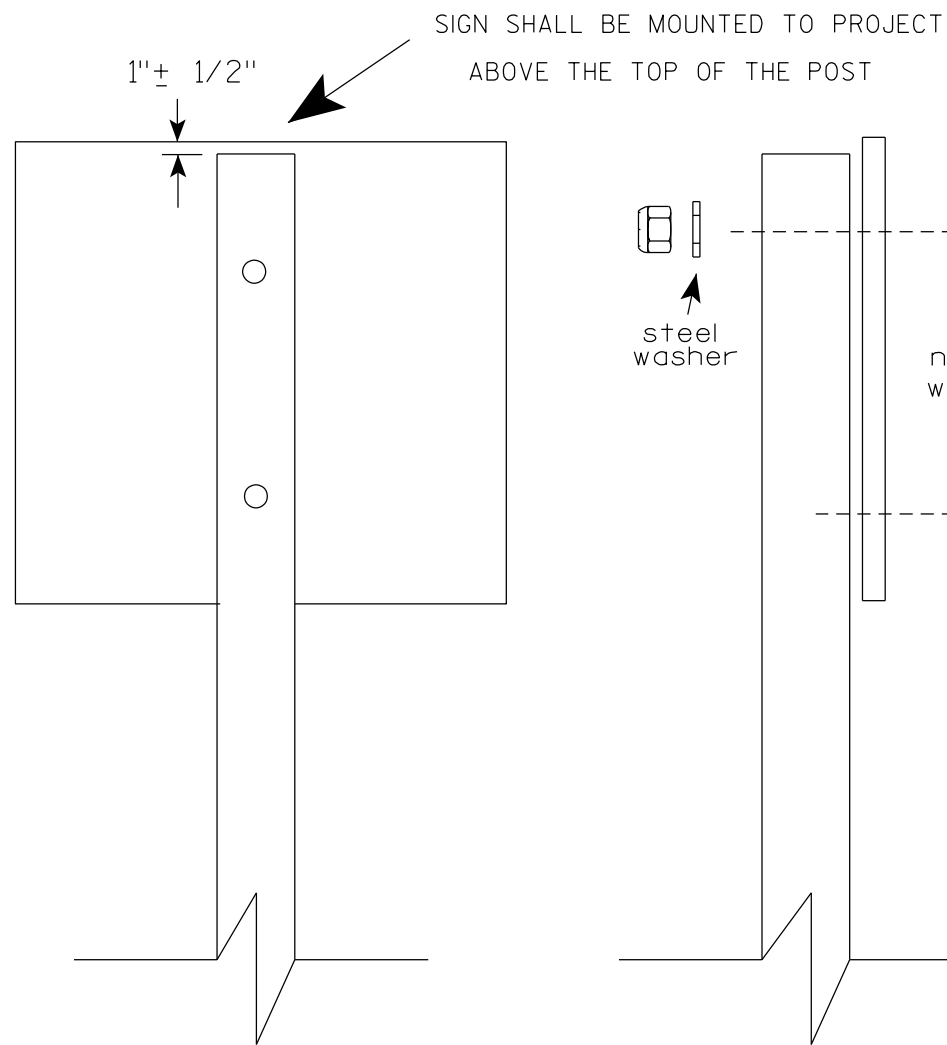
SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)	
L	E
Greater than 108" to 144"	12"

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS

WISCONSIN DEPT OF TRANSPORTATION
 APPROVED *Matthew R. Rauch*
 For State Traffic Engineer
 DATE 8/21/17 PLATE NO. A4-4.15



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

- a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
- b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)

MACHINE BOLTS - 5/16" X 1-3/4" Length w/ lock nuts

WOOD POSTS (4" x 6")

LAG SCREWS - 3/8" X 3" (NO STRINGERS ON BACK OF SIGN)
3/8" X 4" (STRINGERS ON BACK OF SIGN)

SQUARE STEEL POSTS (2" x 2")

MACHINE BOLTS - 3/8" X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)
3/8" X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)

RIVETS - 9/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH

WASHERS (ALL POSTS) -

1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL

1-1/4" O.D. X 3/8" I.D. X .080 NYLON

* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS
TO POSTS

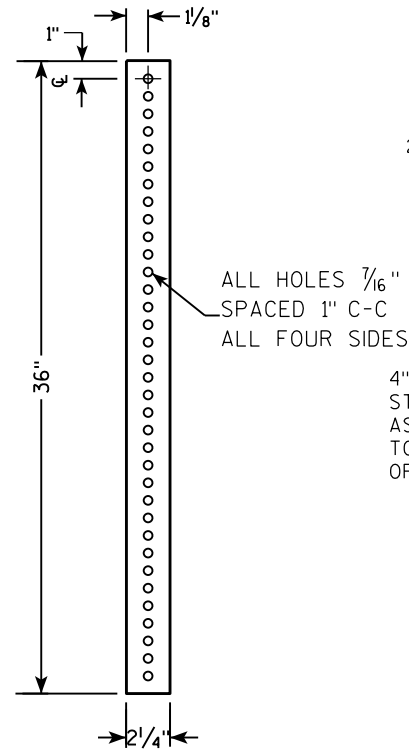
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Rauch*
For State Traffic Engineer

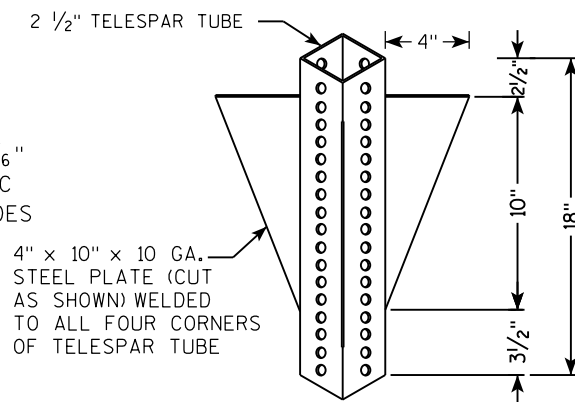
DATE 4/1/2020 PLATE NO. A4-8.9

**TELESCOPIC TUBING ANCHORS
TWO PIECE SYSTEM**

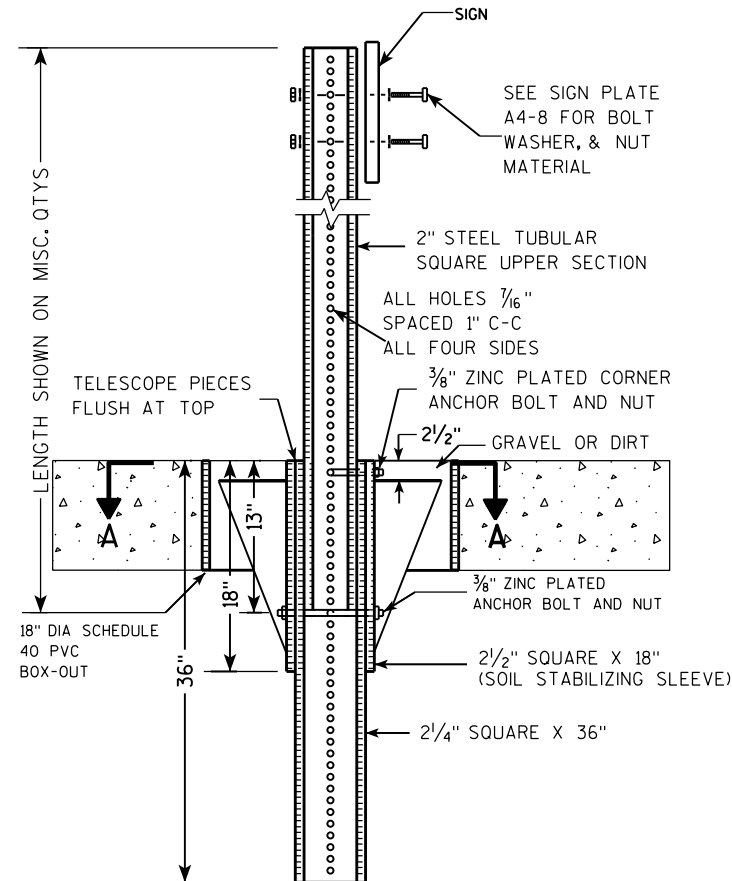
**2 1/4" SQUARE
12 GAUGE
PERFORATED
GALVANIZED FINISH**



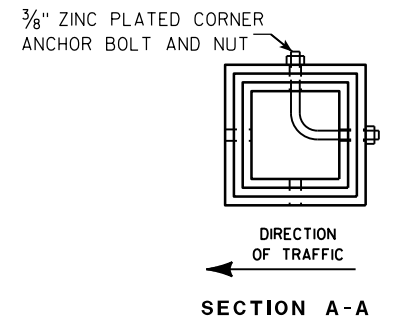
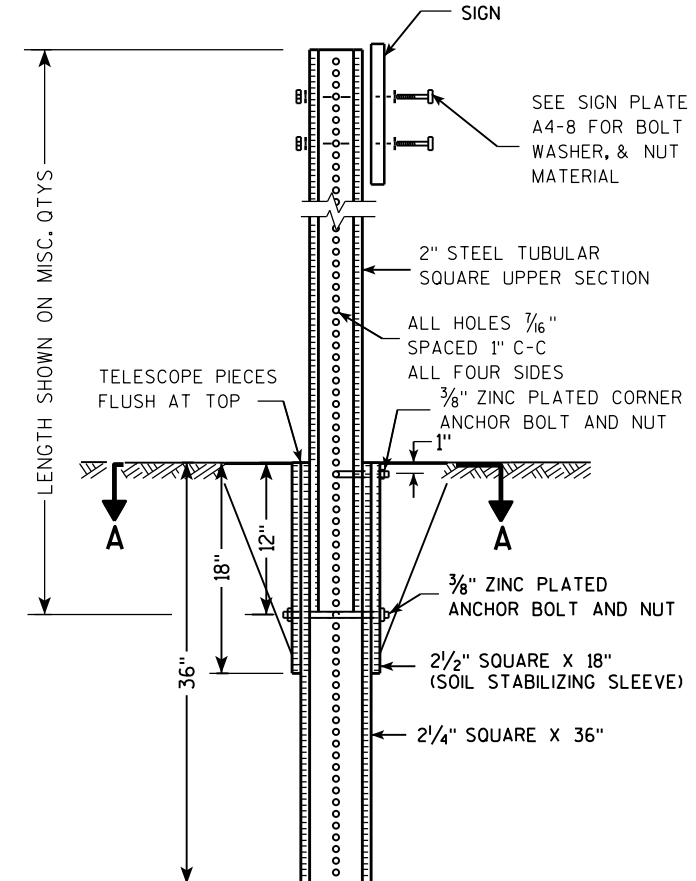
**2 1/2" SQUARE
12 GAUGE
OMNI-DIRECTIONAL
PERFORATED
SOIL STABILIZING SLEEVE
GALVANIZED FINISH**



**DETAIL OF TUBULAR STEEL SIGN POST
(IN POURED CONCRETE OR ASPHALT)**



**DETAIL OF TUBULAR STEEL SIGN POST
(IN LOCATIONS OTHER THAN POURED CONCRETE OR ASPHALT)**



Area of Sign Installation (Sq. Ft.)	Number of Required Posts
9 or less	1
Greater than 9 less than or equal to 18	2
Greater than 18 less than or equal to 27	3

Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

**TUBULAR STEEL
SIGN POST
A4-9**

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 2/05/15 PLATE NO. A4-9.9

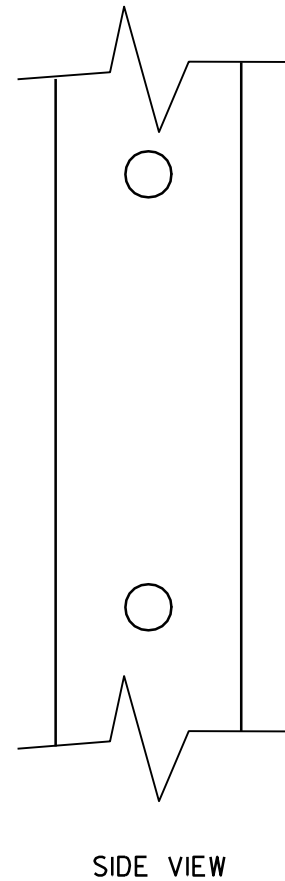
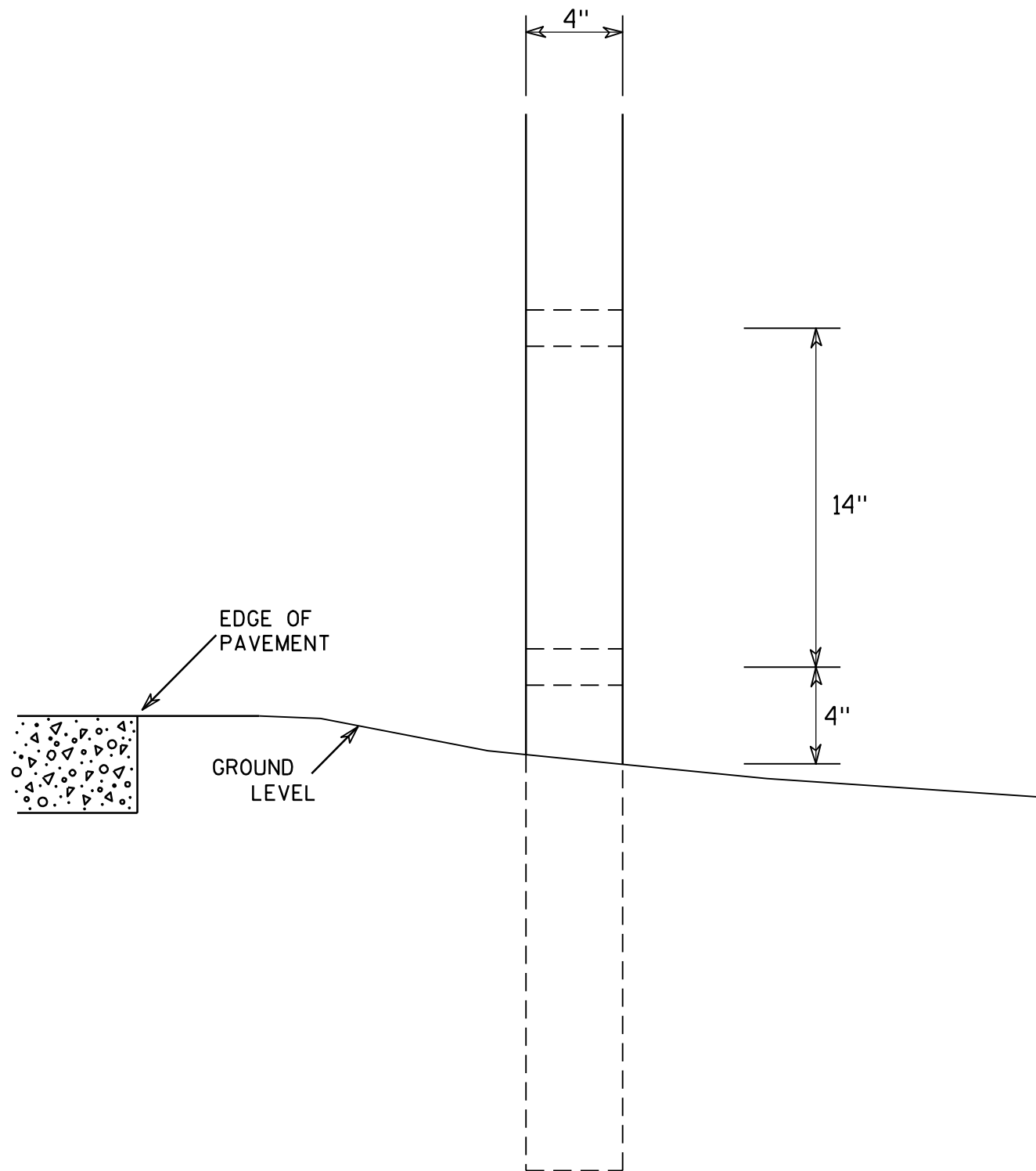
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E



GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

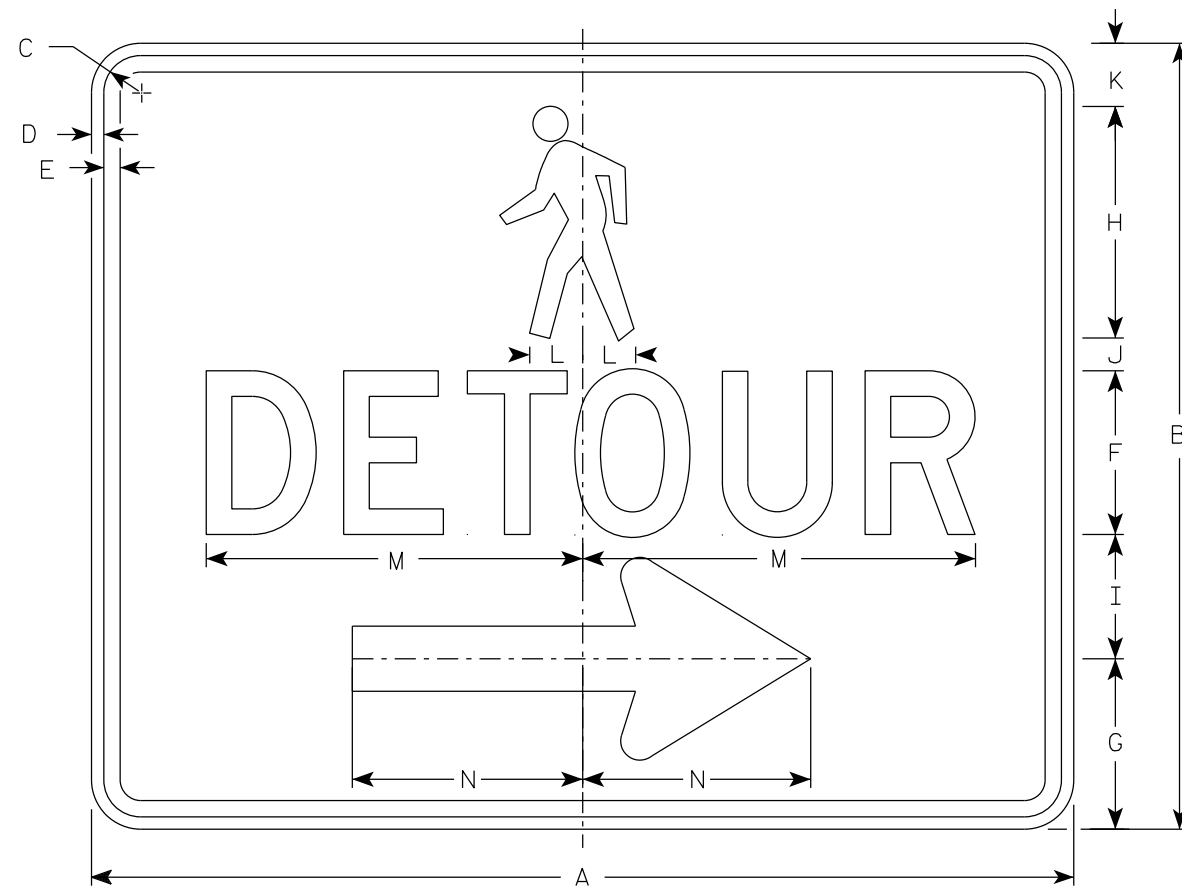
7

7

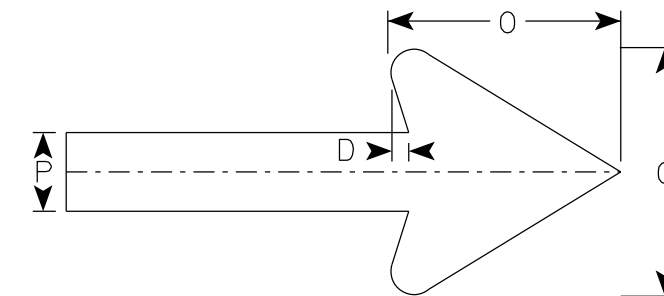
4 X 6 WOOD POST MODIFICATIONS	
<i>WISCONSIN DEPT OF TRANSPORTATION</i>	
APPROVED	<i>Chester J Spang</i> for State Traffic Engineer
DATE <u>3/27/97</u>	PLATE NO. <u>A4-11.2</u>

NOTES

1. Sign is Type II-Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. M4-9BL is the same as M4-9BR except the arrow is reversed.



M4 - 9BR



Arrow Detail

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	30	24	1 1/8	3/8	1/2	5	5 1/4	7 1/8	3 3/4	1	1 1/8	1 5/8	11 3/4	7	6	2											5.00
3																											
4																											
5																											

STANDARD SIGN
M4-9B L&R

WISCONSIN DEPT OF TRANSPORTATION

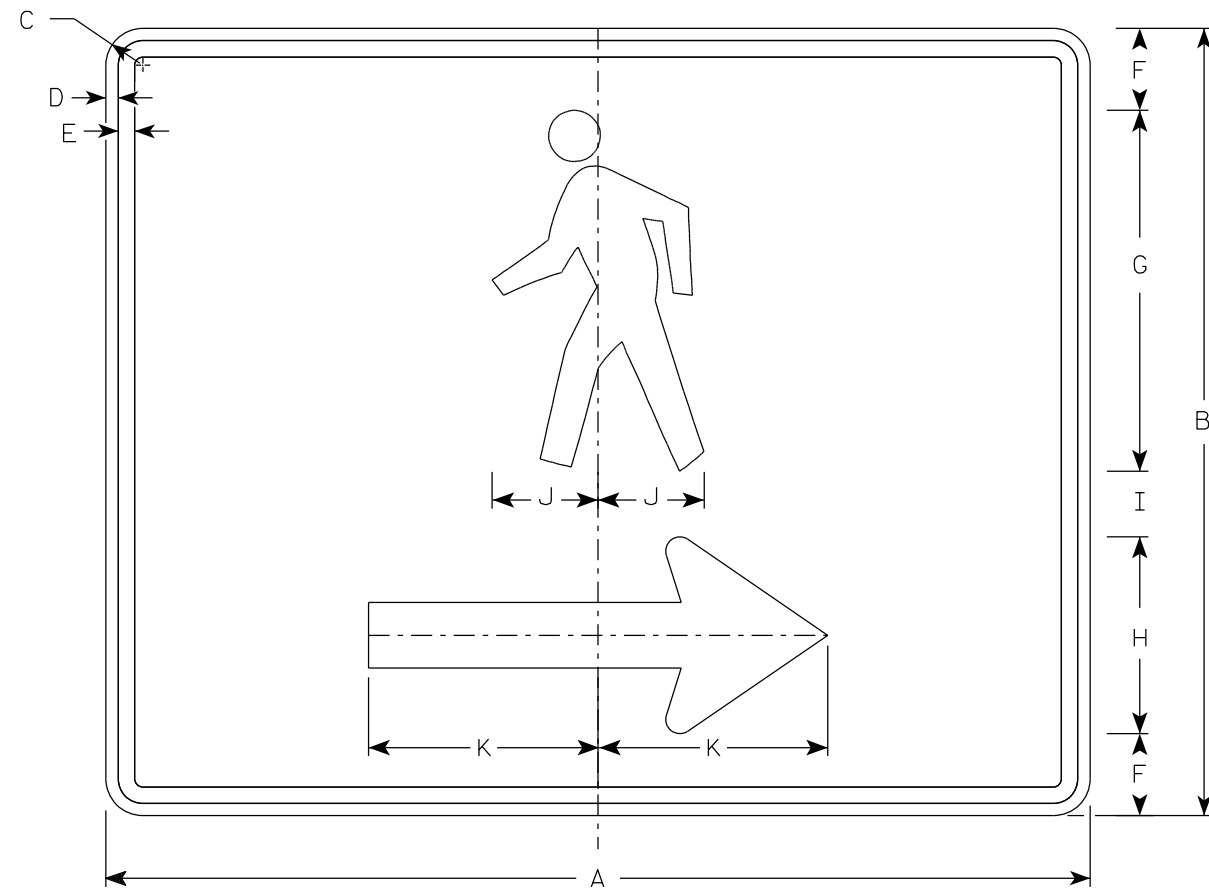
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 7/1/19 PLATE NO. M4-9B.2

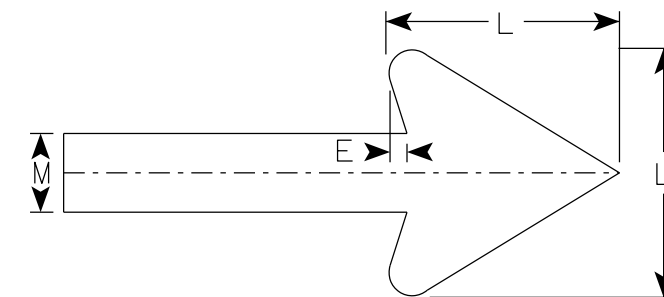
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II- Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. M4-60L is the same as M4-60R except the arrow is reversed.



M4-60R

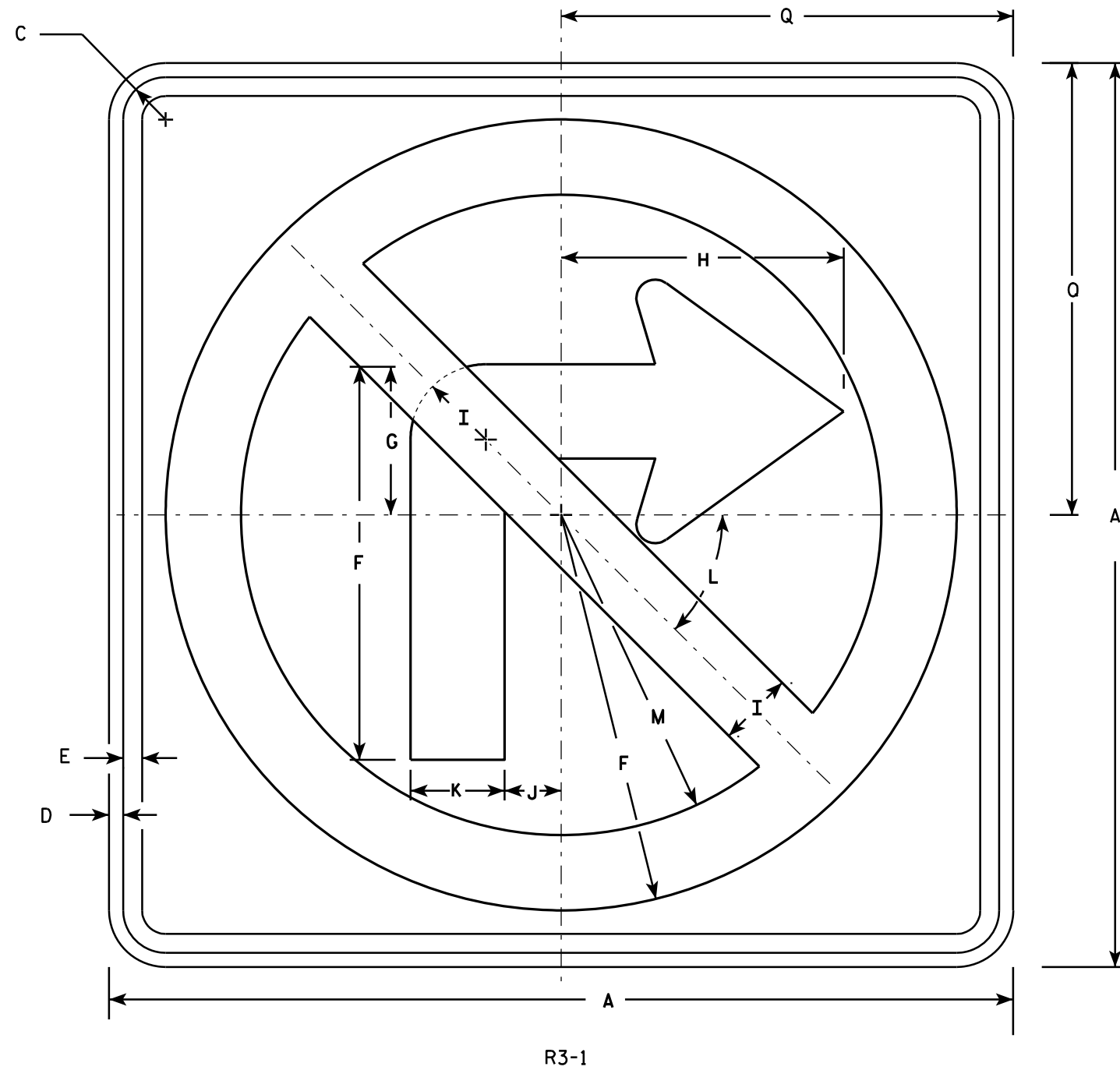


Arrow Detail

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	30	24	1 1/8	3/8	1/2	2 1/2	11	6	2	3 1/4	7	6	2														5.00
3																											
4																											
5																											

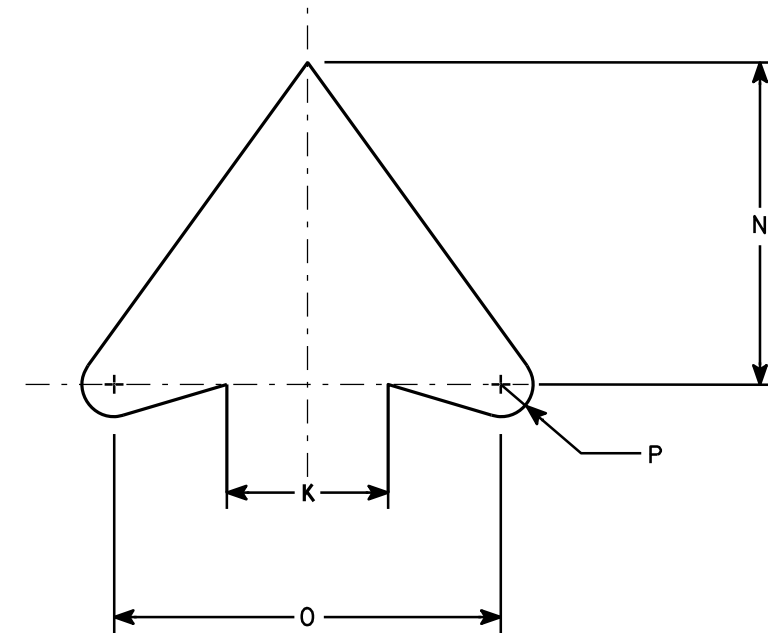
STANDARD SIGN
M4-60 L&R

WISCONSIN DEPT OF TRANSPORTATION
APPROVED *Matthew R Rauch*
For State Traffic Engineer
DATE 9/16/2021 PLATE NO. M4-60.1



NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - See note 4
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. Border & Arrow are non reflective black, the circle with diagonal bar is reflective red.



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2	10 1/2	4	7 1/2	2	1 1/2	2 1/2	45	8 1/2	5	6	1/2	12										4.0
2S	24		1 1/8	3/8	1/2	10 1/2	4	7 1/2	2	1 1/2	2 1/2	45°	8 1/2	5	6	1/2	12										4.0
2M	36		1 5/8	5/8	3/4	15 3/4	6	11 1/4	3	2 1/4	3 3/4	45	12 3/4	7 1/2	9	3/4	18										9.0
3	36		1 5/8	5/8	3/4	15 3/4	6	11 1/4	3	2 1/4	3 3/4	45	12 3/4	7 1/2	9	3/4	18										9.0
4	36		1 5/8	5/8	3/4	15 3/4	6	11 1/4	3	2 1/4	3 3/4	45°	12 3/4	7 1/2	9	3/4	18										9.0
5	48		2 1/4	3/4	1	21	8	15	4	3	5	45°	17	10	12	1	24										16.0

STANDARD SIGN
R3-1

WISCONSIN DEPT OF TRANSPORTATION

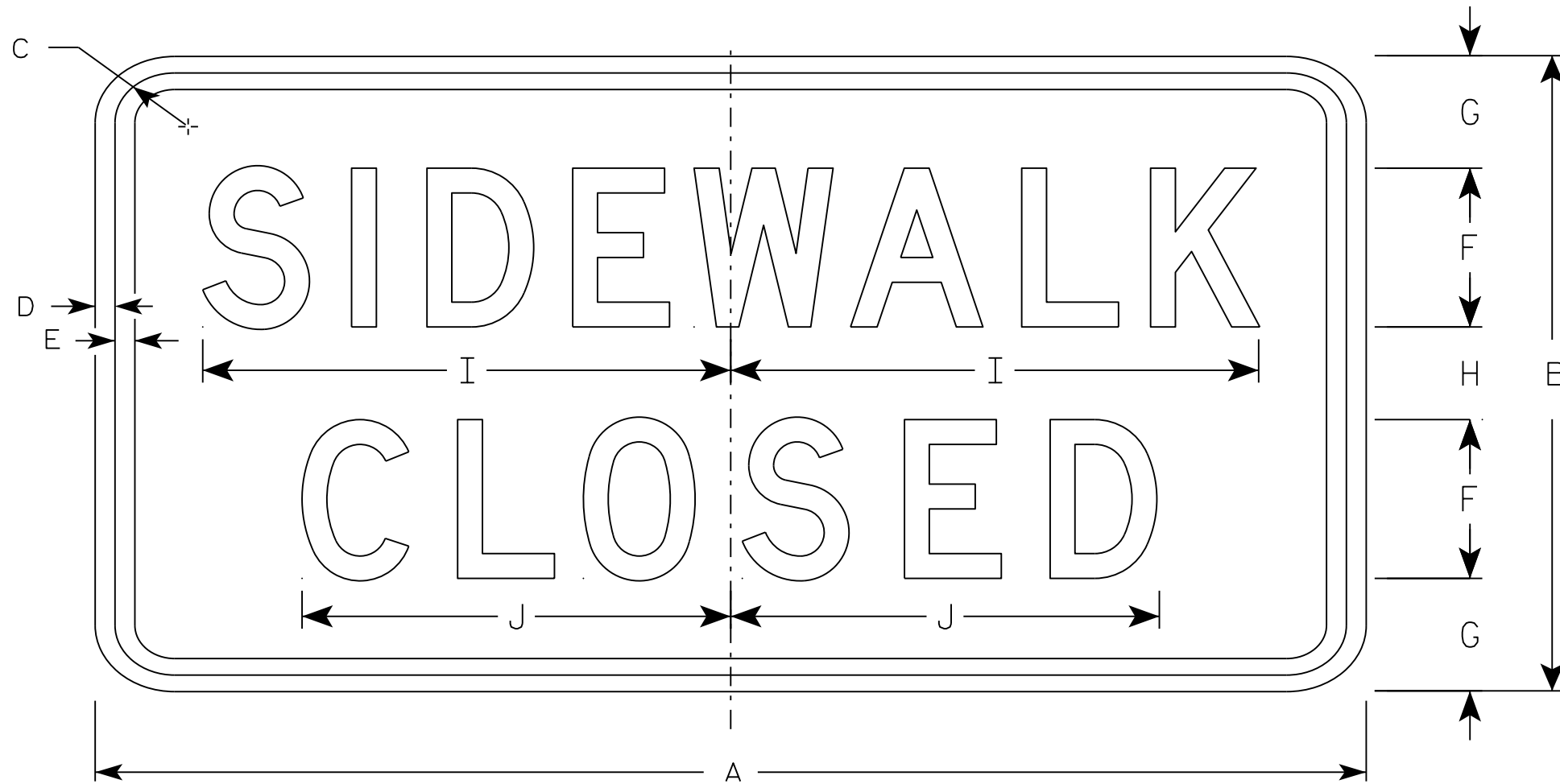
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 12/08/10 PLATE NO. R3-1.5

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Use Size 2 for Sidewalks. Use Size 3 for Paths and Trails.



R9-9

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	12	1 3/4	1/2	1/2	3	2 1/8	1 3/4	10	8 1/8																	2.0
2M	24	12	1 3/4	1/2	1/2	3	2 1/8	1 3/4	10	8 1/8																	2.0
3	30	18	1 3/4	1/2	1/2	4	3 1/2	3	12 1/2	10 1/4																	3.75
4																											
5																											

STANDARD SIGN
R9-9

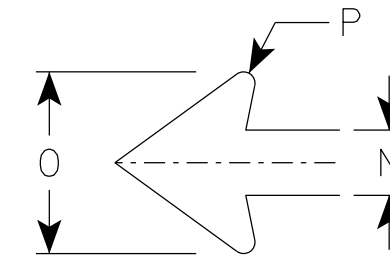
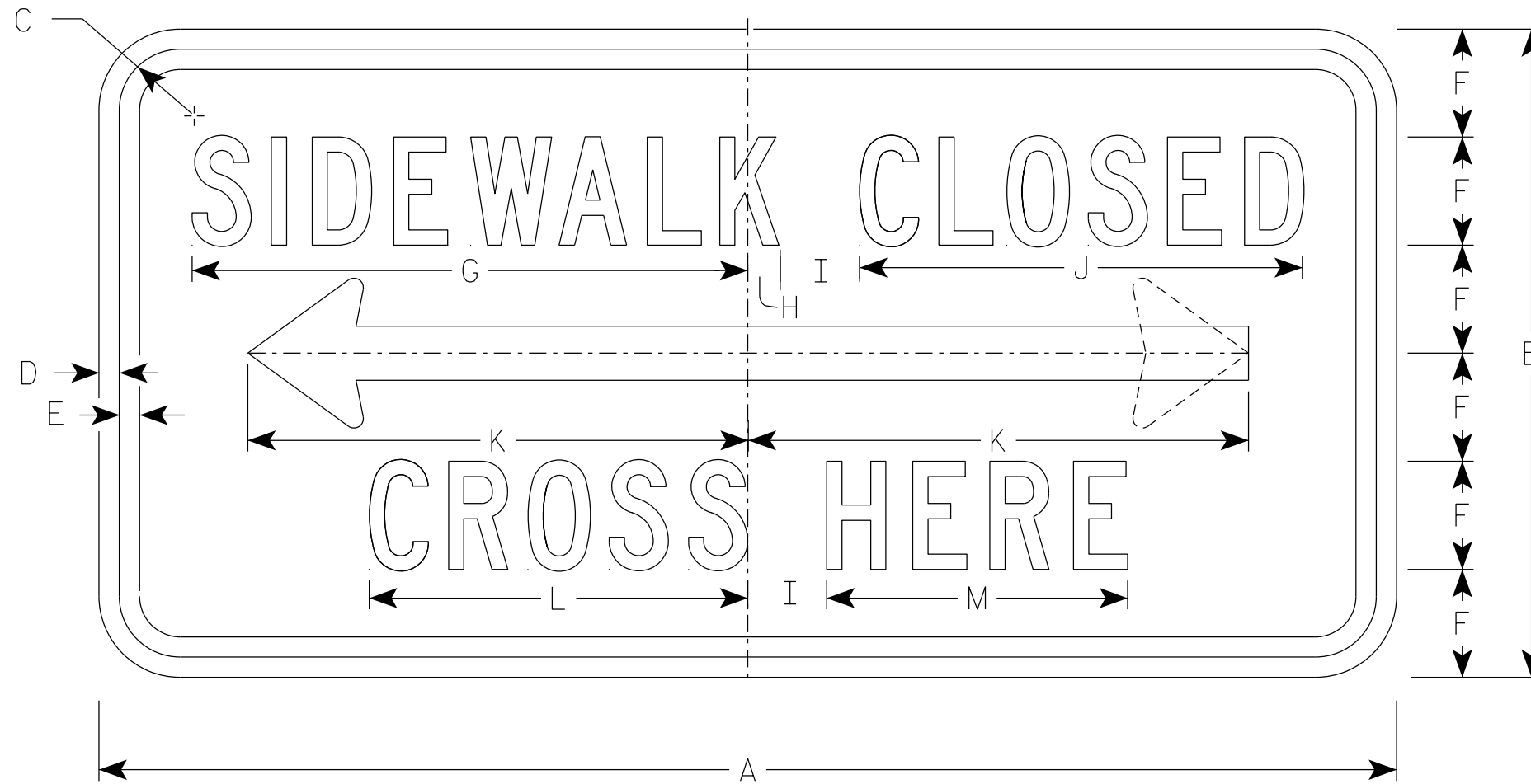
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 8/11/16 PLATE NO. R9-9.6

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Use Size 2 for Sidewalks. Use Size 3 for paths and Trails.
6. R9-11AD (double arrow)
R9-11AL (left arrow)
R9-11AR (right arrow)



ARROW DETAIL

R9-11A

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	12	1 1/8	3/8	3/8	2	10 1/4	5/8	1 1/2	8 1/4	9 1/4	7	5 5/8	1	2 3/4	1/8											2.0
2M	24	12	1 1/8	3/8	3/8	2	10 1/4	5/8	1 1/2	8 1/4	9 1/4	7	5 5/8	1	2 3/4	1/8											2.0
3	30	15	1 1/8	3/8	1/2	2 1/2	12 3/4	1/2	2	10 1/4	12 3/8	8 5/8	6 3/4	1 1/4	3 5/8	1/4											3.125
4																											
5																											

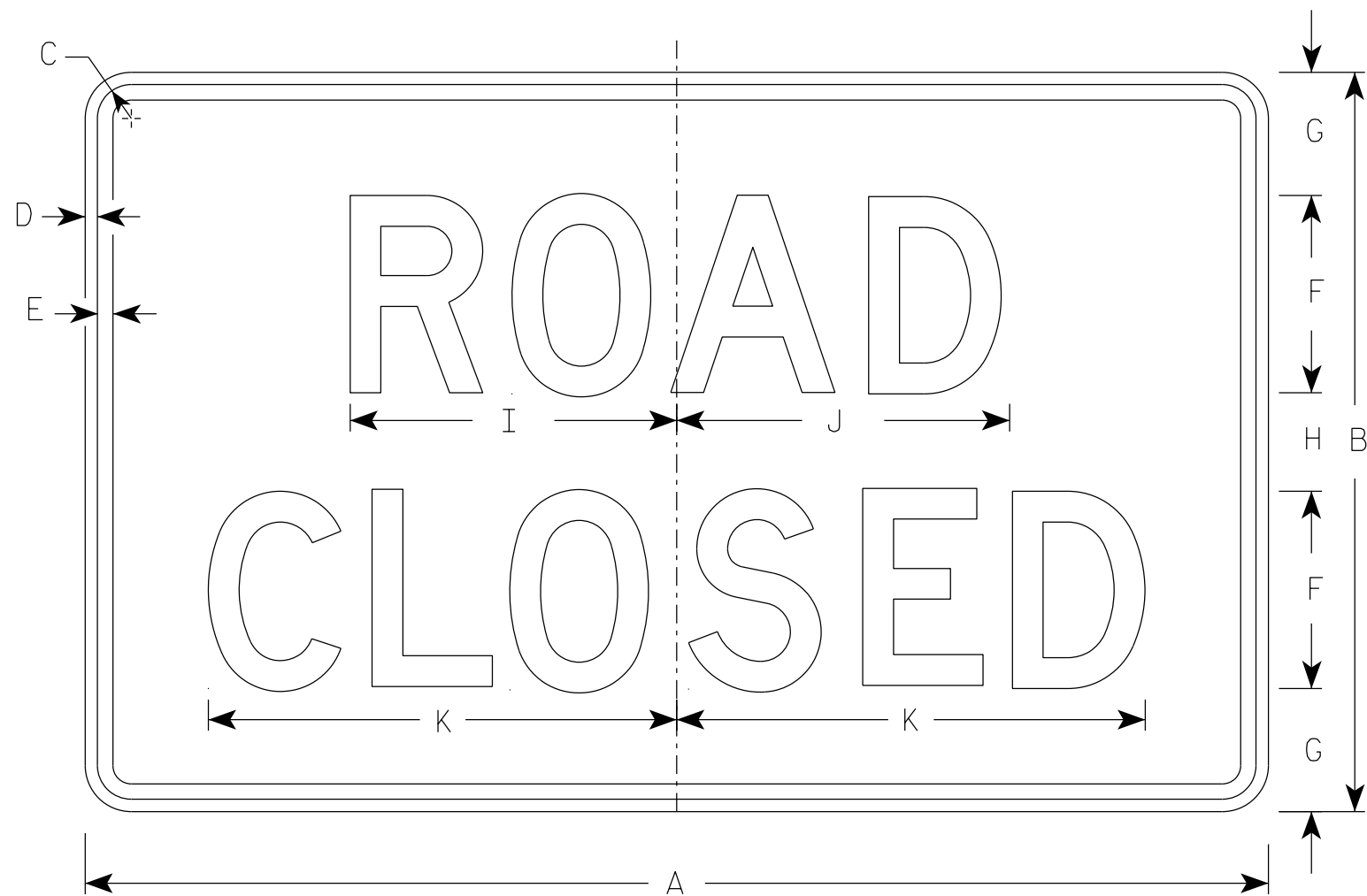
STANDARD SIGN
R9-11A

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 3/31/2021 PLATE NO. R9-11A.5

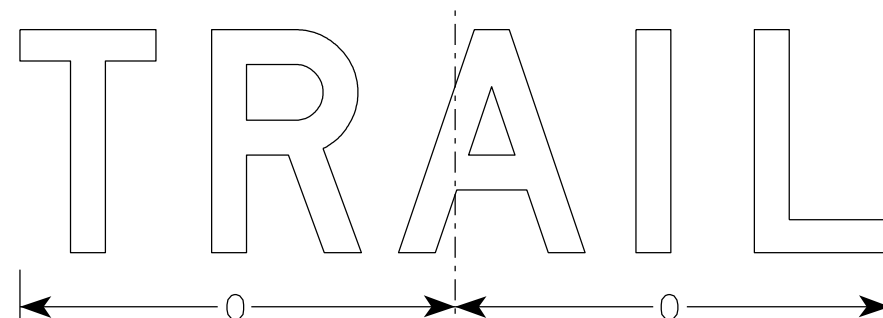
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**



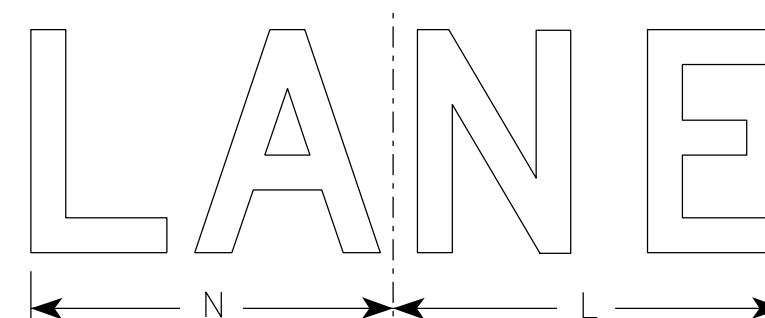
R11-2



R11-2R



R11-2T



R11-2L

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Modify the message as required.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
2M	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
3	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
4	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
5	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0

STANDARD SIGN
R11-2

WISCONSIN DEPT OF TRANSPORTATION

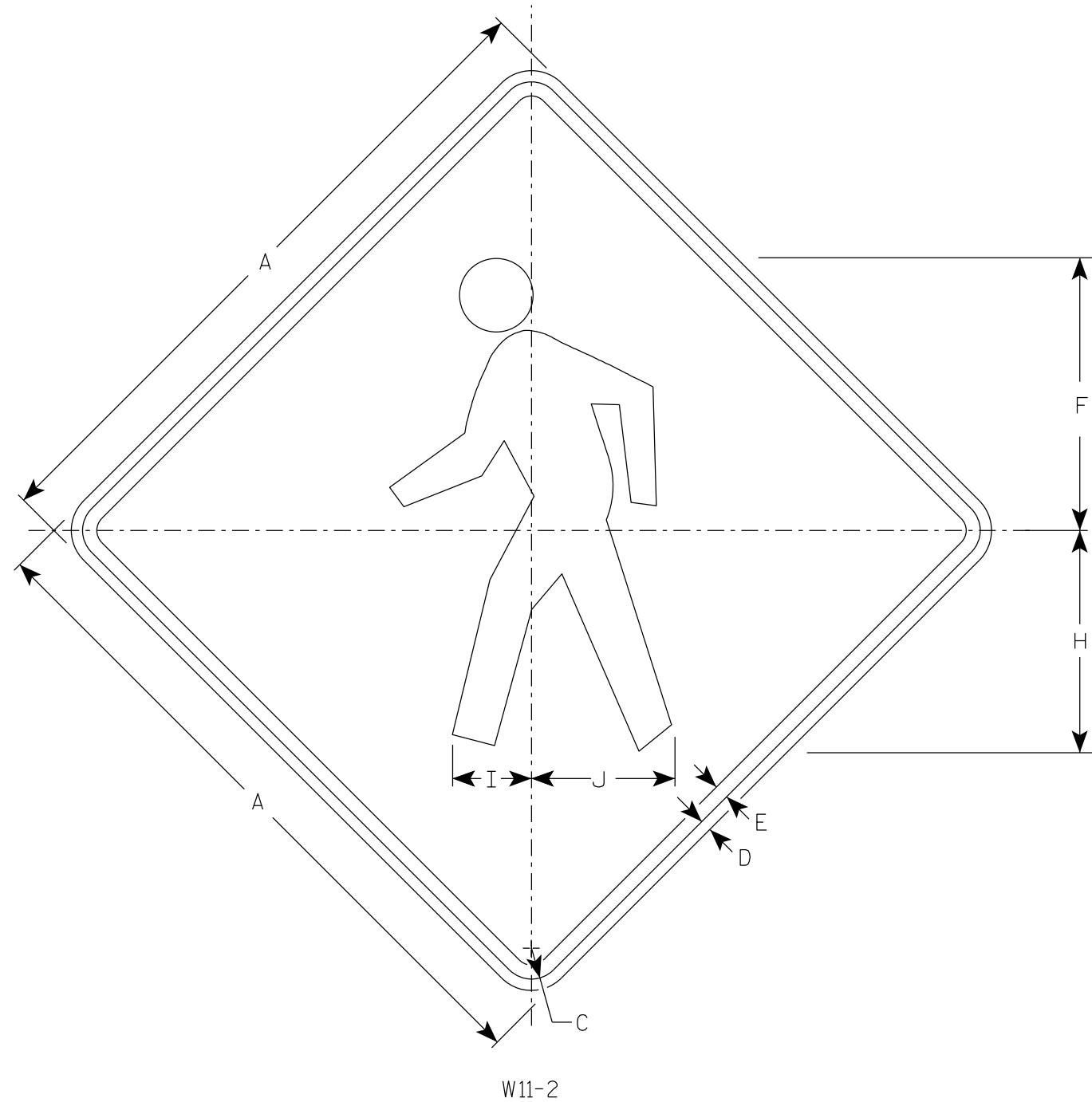
APPROVED *Matthew R Rauch*
For State Traffic Engineer

DATE 3/29/2021 PLATE NO. R11-2.11

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
 Background - Yellow
 Message - Black



W11-2

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2	9 3/4		7 7/8	2 7/8	5 1/8																	4.0
2S	30		1 3/8	1/2	5/8	12 1/8		9 7/8	3 1/2	6 3/8																	6.25
2M	36		1 5/8	5/8	3/4	14 1/2		11 7/8	4 1/4	7 5/8																	9.0
3	36		1 5/8	5/8	3/4	14 1/2		11 7/8	4 1/4	7 5/8																	9.0
4	48		2 1/4	3/4	1	19 3/8		15 3/4	5 5/8	10 1/4																	16.0
5																											

STANDARD SIGN
W11-2

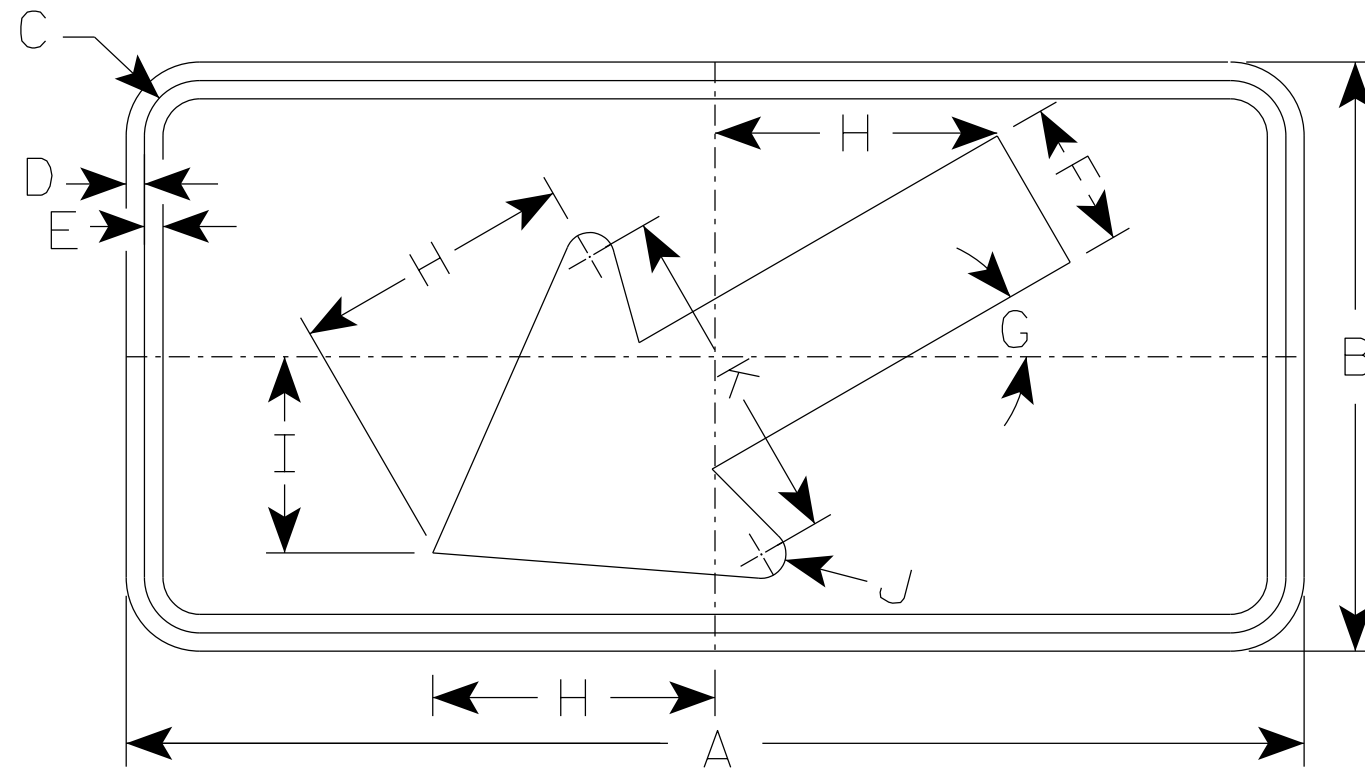
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Rauch*
for State Traffic Engineer

DATE 4/8/2020 PLATE NO. W11-2.8

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Yellow
Message - Black
3. W16-7R is the same as W16-7L
except the arrow is reversed along
the vertical centerline.



W16-7L

- * For 36" x 36" Warning Signs, use 30" x 18" W16-7L signs.
- * For 48" x 48" Warning Signs, use 48" x 24" W16-7L signs.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	12	1 1/8	3/8	3/8	3	30°	5 3/4	4	1/2	7																2.0
* 2M	30	18	1 1/8	3/8	1/2	4 1/2	30°	8 1/2	6	5/8	10 1/4																3.75
* 3	30	18	1 1/8	3/8	1/2	4 1/2	30°	8 1/2	6	5/8	10 1/4																3.75
* 4	48	24	1 3/8	1/2	5/8	6	30°	11 1/2	8	1	14																8.0
5																											

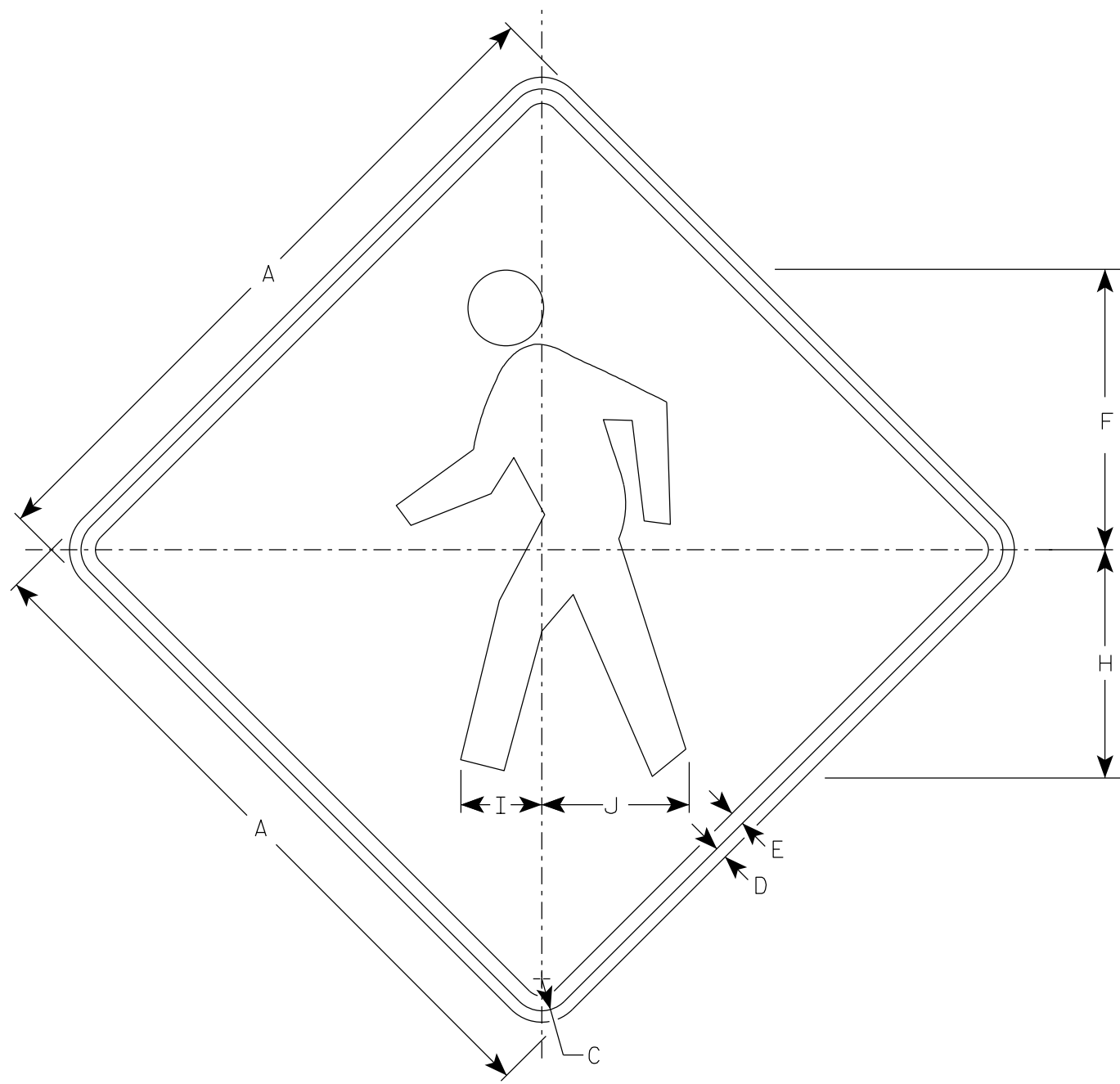
STANDARD SIGN
W16-7

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 3/16/2021 PLATE NO. W16-7.8

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**



W011-2

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	14 1/2		11 7/8	4 1/4	7 5/8																	9.0
2S	48		2 1/4	3/4	1	19 3/8		15 3/4	5 5/8	10 1/4																	16.0
2M	48		2 1/4	3/4	1	19 3/8		15 3/4	5 5/8	10 1/4																	16.0
3	48		2 1/4	3/4	1	19 3/8		15 3/4	5 5/8	10 1/4																	16.0
4	48		2 1/4	3/4	1	19 3/8		15 3/4	5 5/8	10 1/4																	16.0
5																											

STANDARD SIGN
W011-2

WISCONSIN DEPT OF TRANSPORTATION

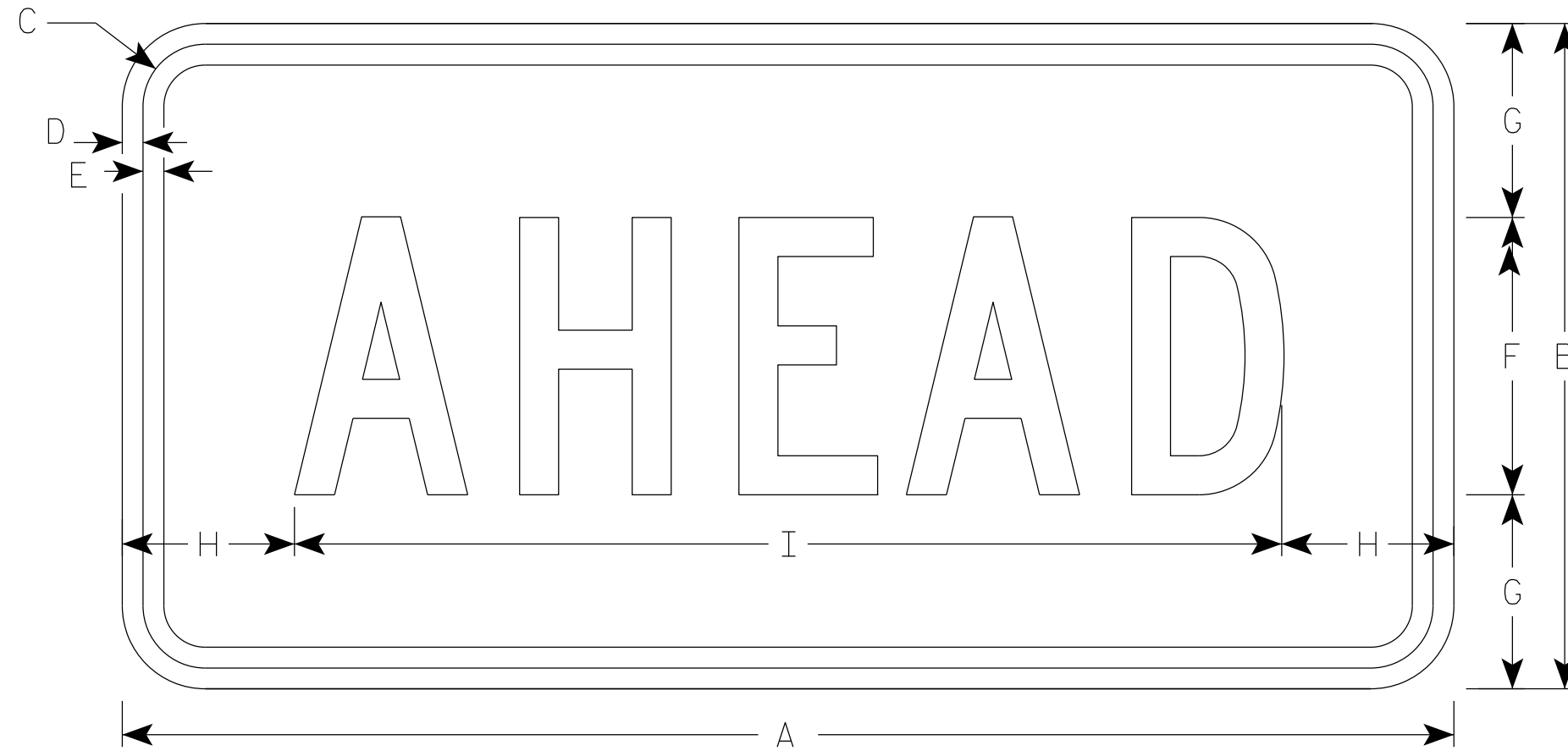
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 4/8/2020 PLATE NO. W011-2.1

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



W016-9P

7

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SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30	18	1 1/8	3/8	1/2	7	5 1/2	2 3/4	24 1/2																		3.75
2S	48	24	1 3/8	1/2	5/8	10	7	6 1/8	35 3/4																		8.0
2M	48	24	1 3/8	1/2	5/8	10	7	6 1/8	35 3/4																		8.0
3	48	24	1 3/8	1/2	5/8	10	7	6 1/8	35 3/4																		8.0
4	48	24	1 3/8	1/2	5/8	10	7	6 1/8	35 3/4																		8.0
5	48	24	1 3/8	1/2	5/8	10	7	6 1/8	35 3/4																		8.0

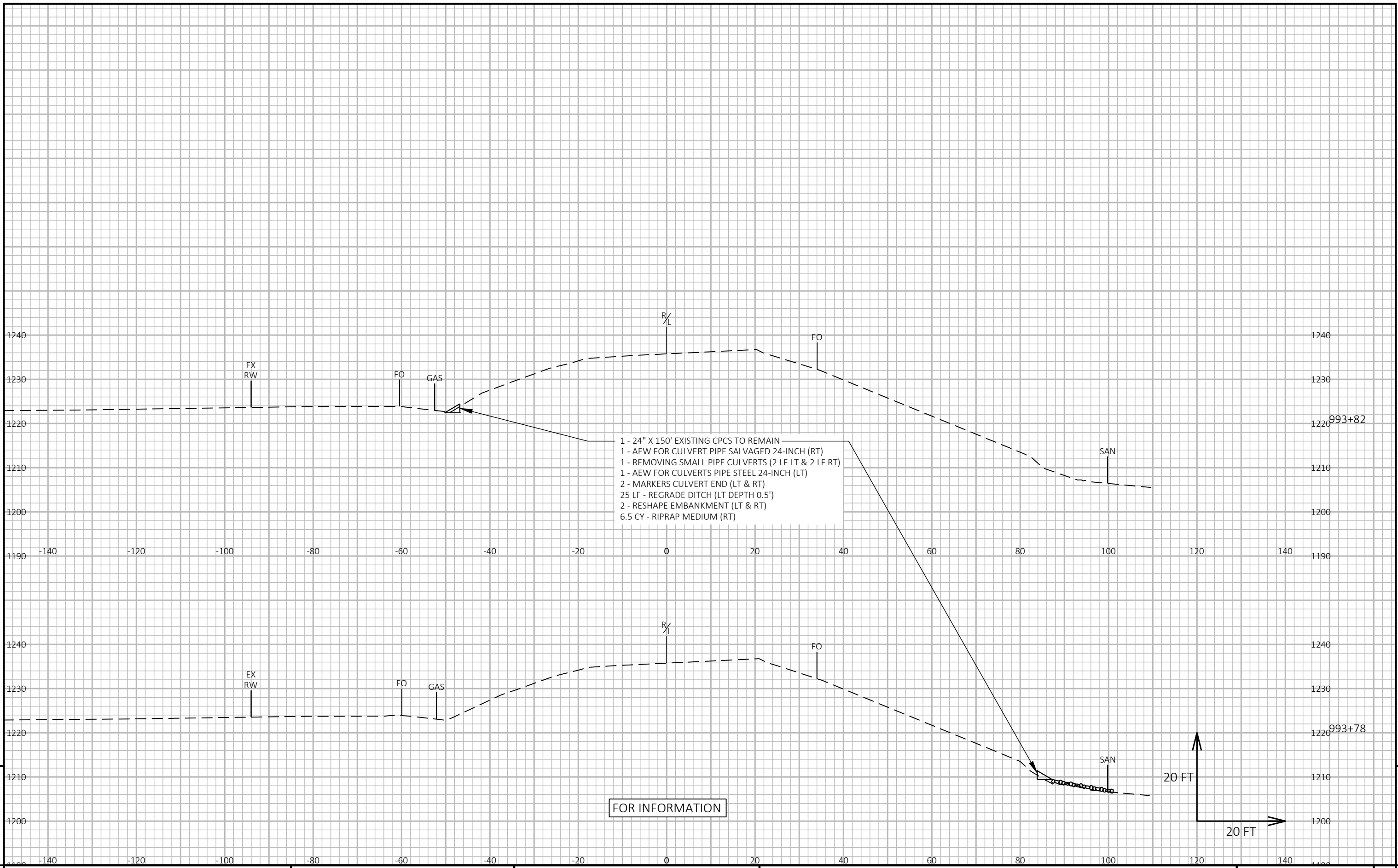
STANDARD SIGN
W016-9P

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

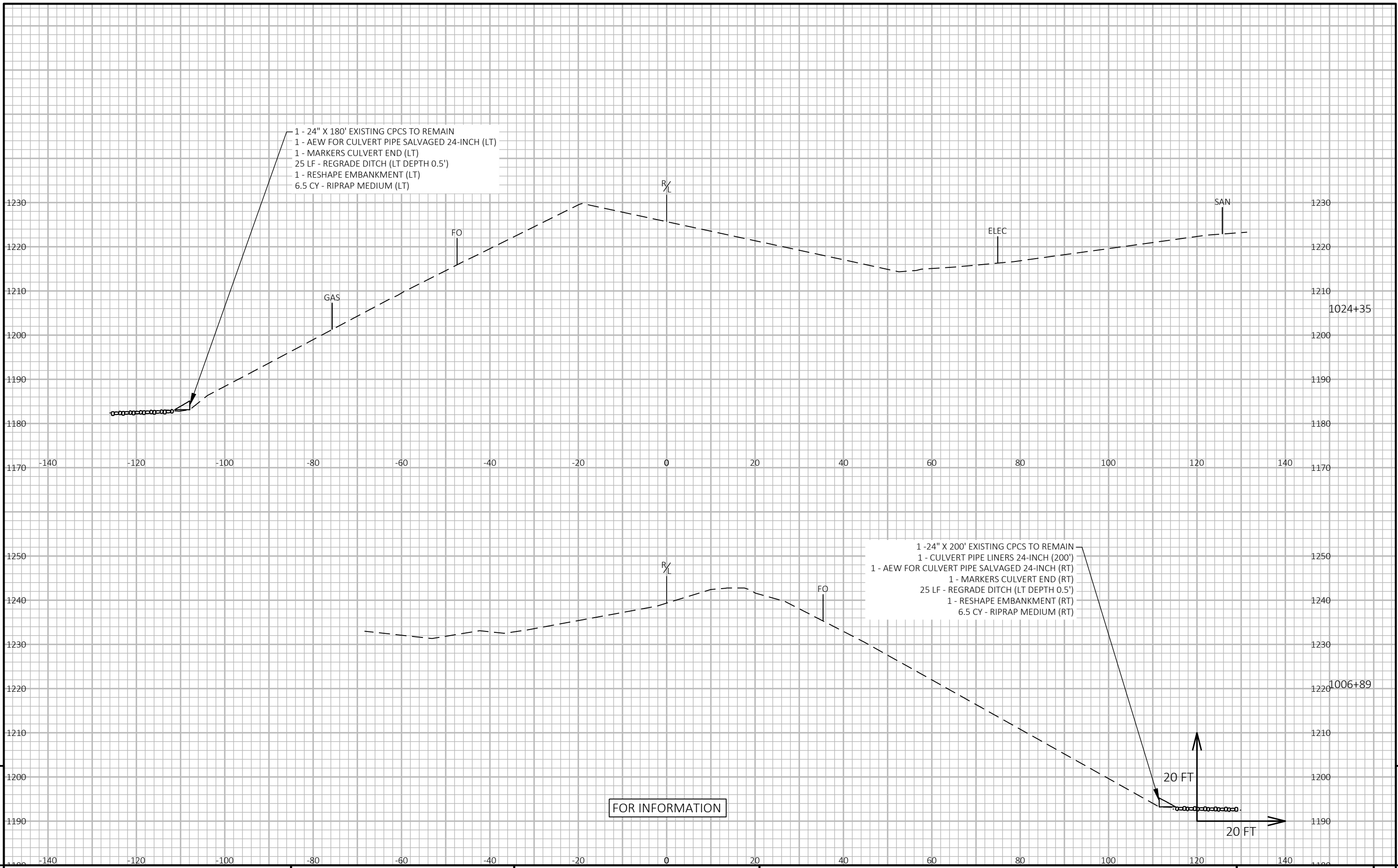
DATE 5/1/19 PLATE NO. W016-9P.7

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**



9

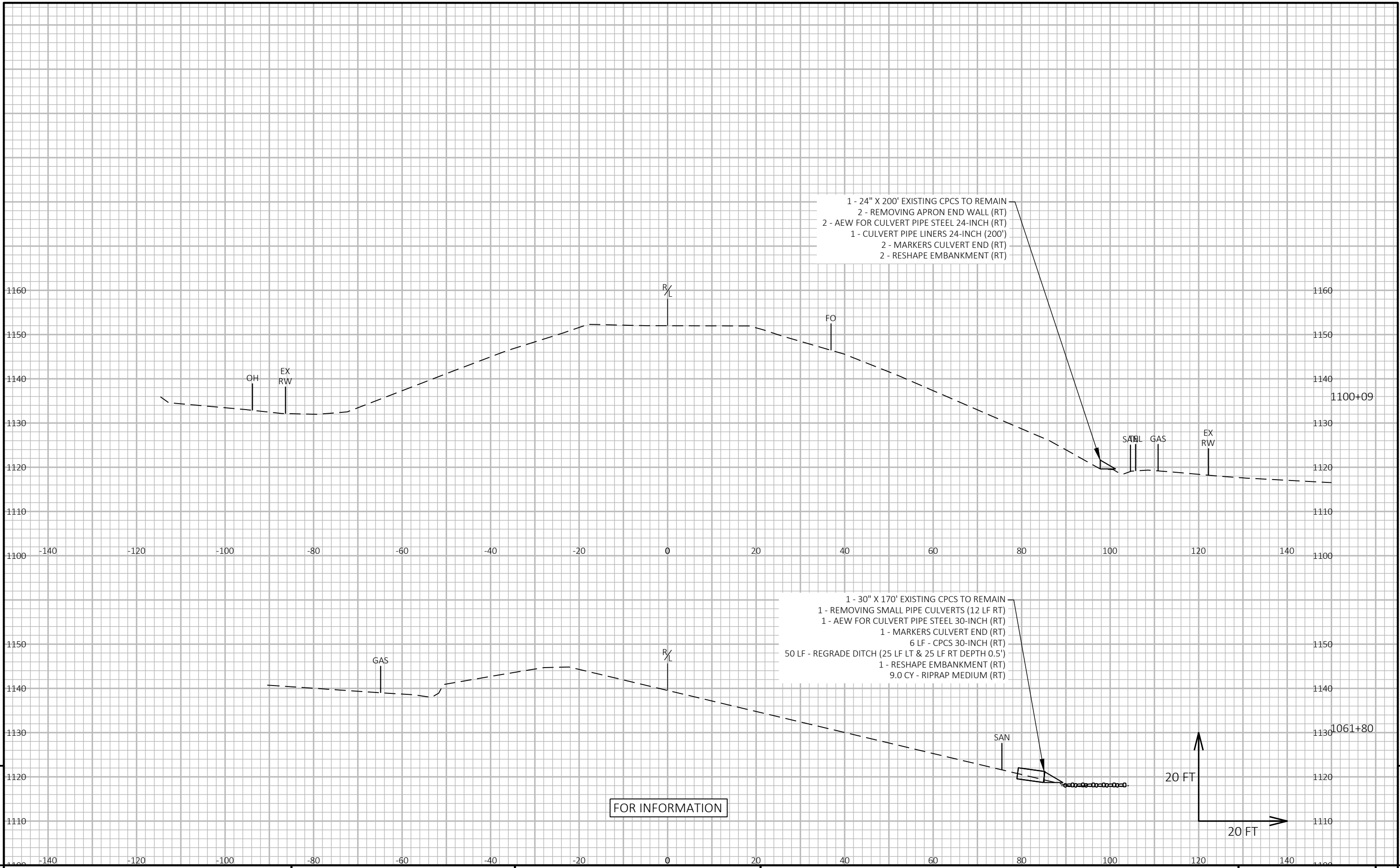
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9

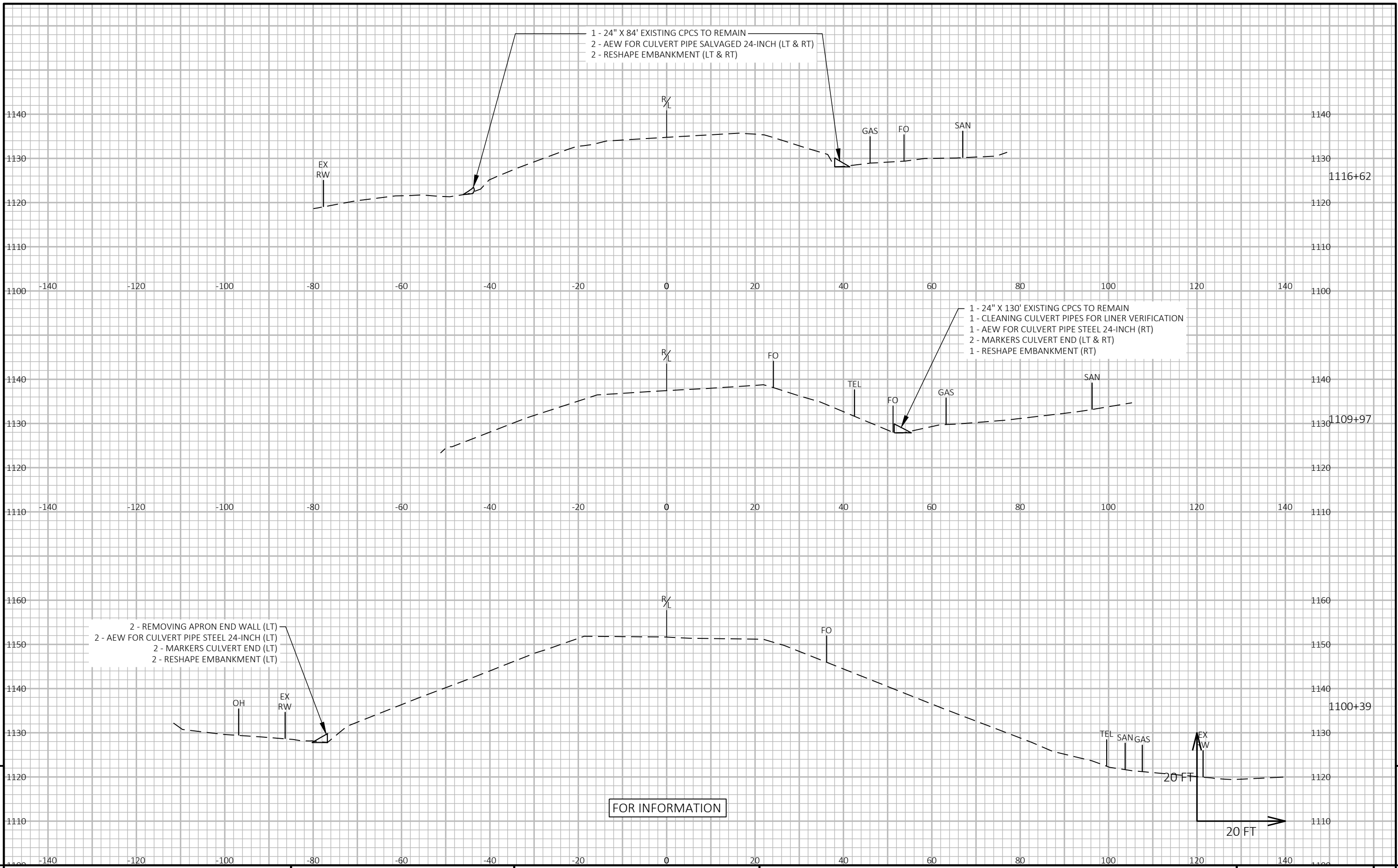
9

PROJECT NO: 5540-02-71	HWY: STH 27	COUNTY: CRAWFORD	CROSS SECTIONS: STH 27 CULVERT END TREATMENTS	SHEET	E
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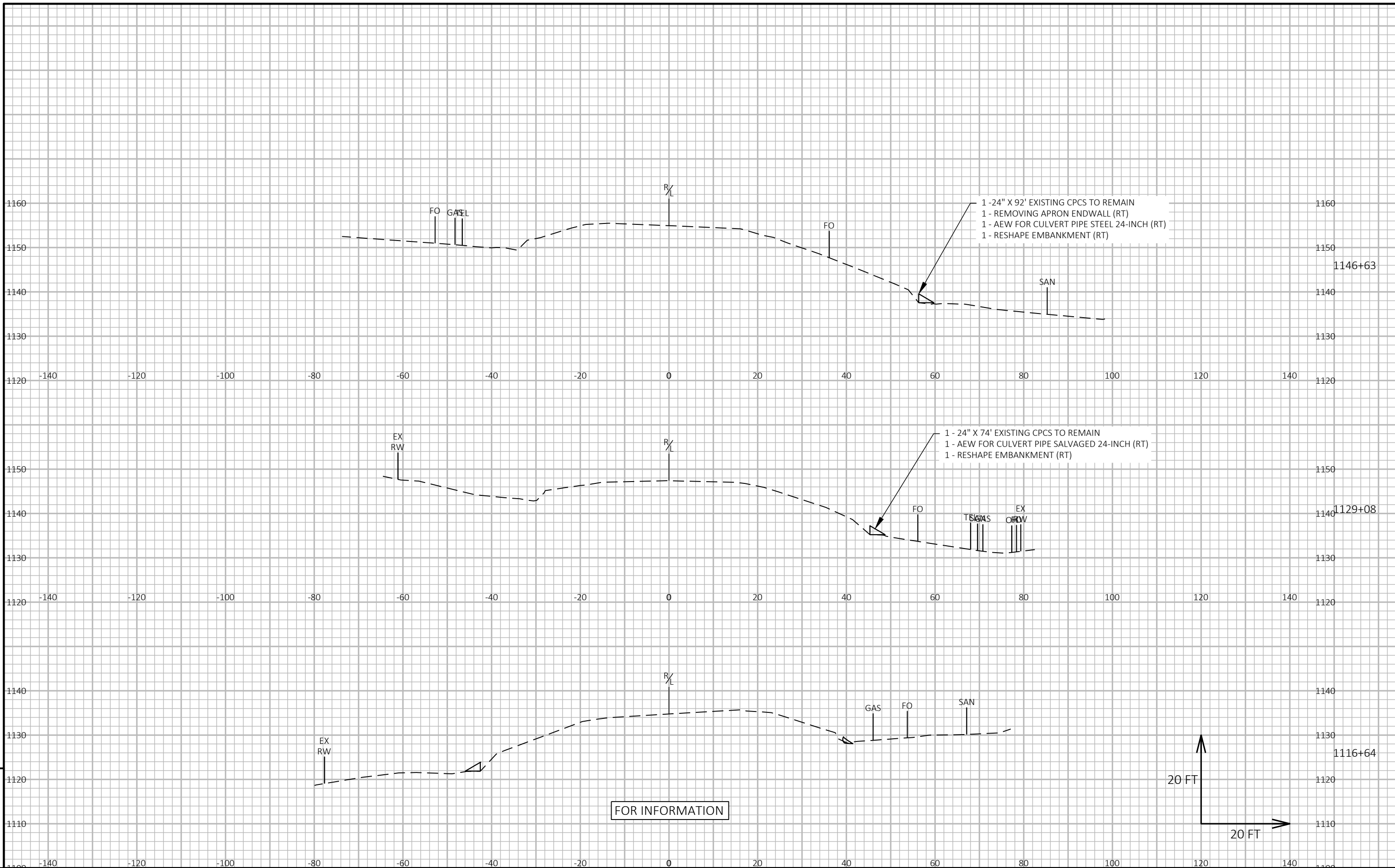
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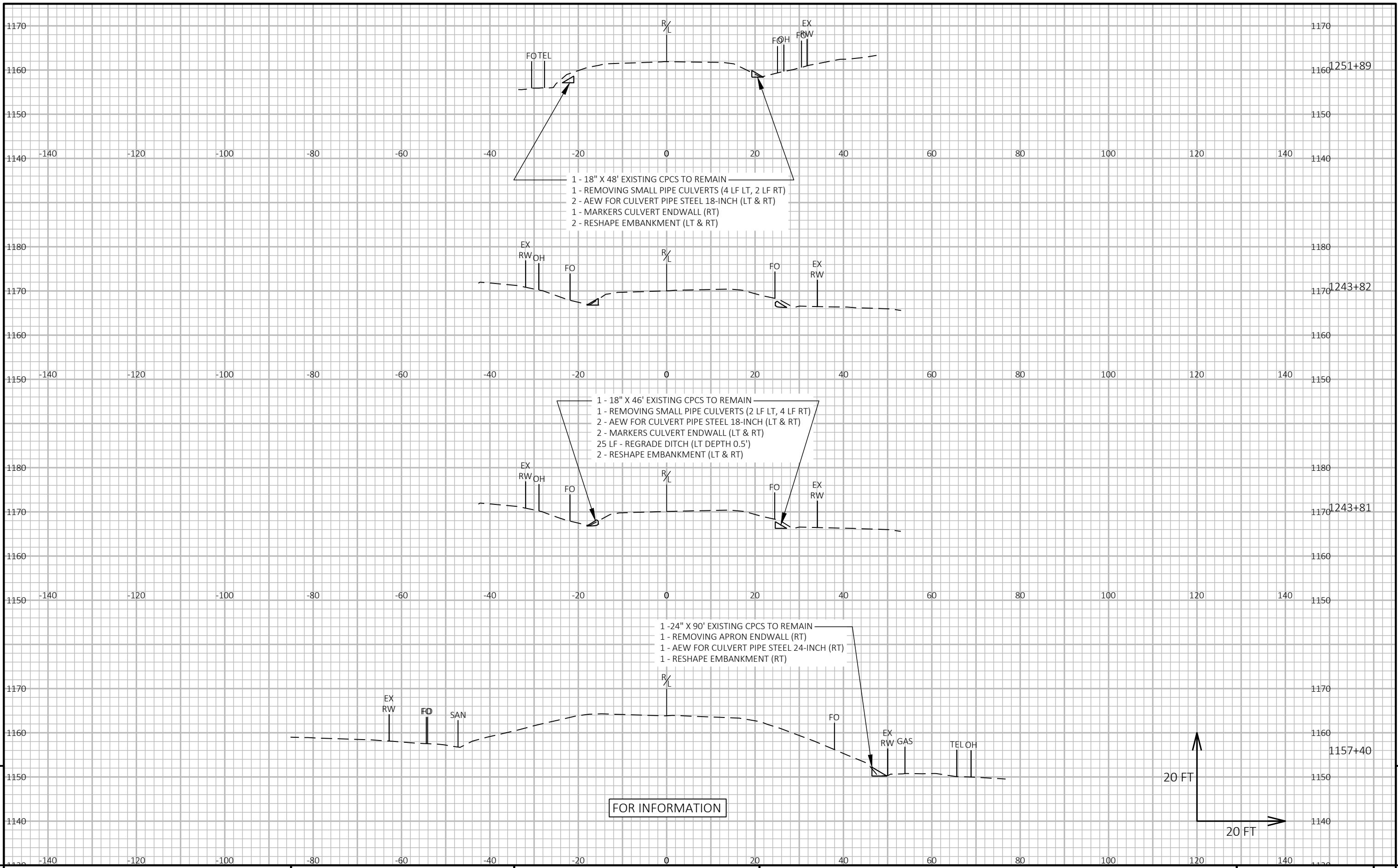
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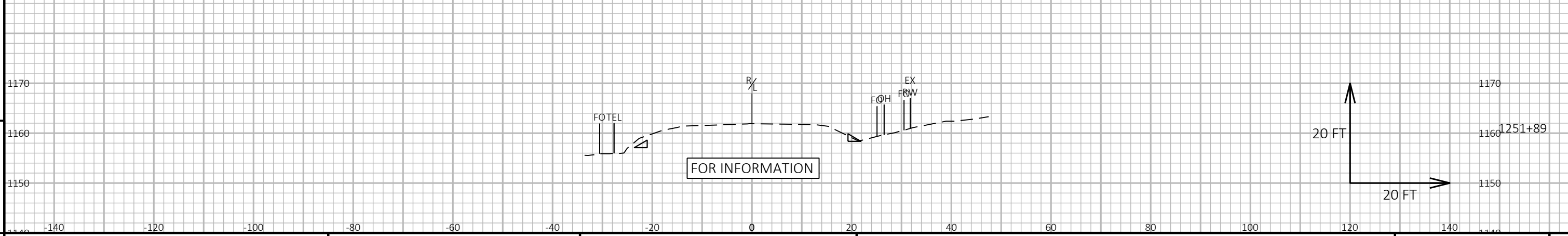
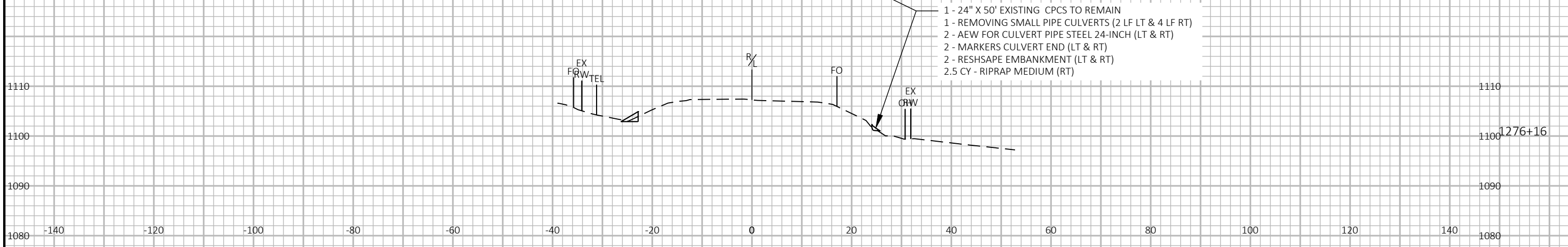
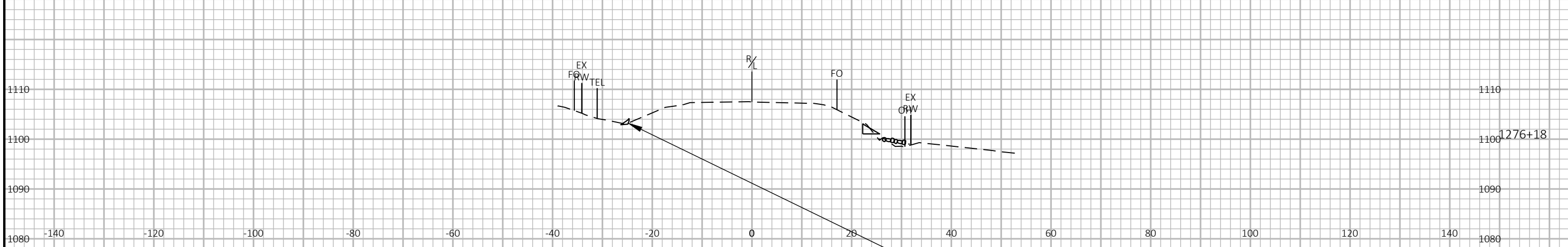
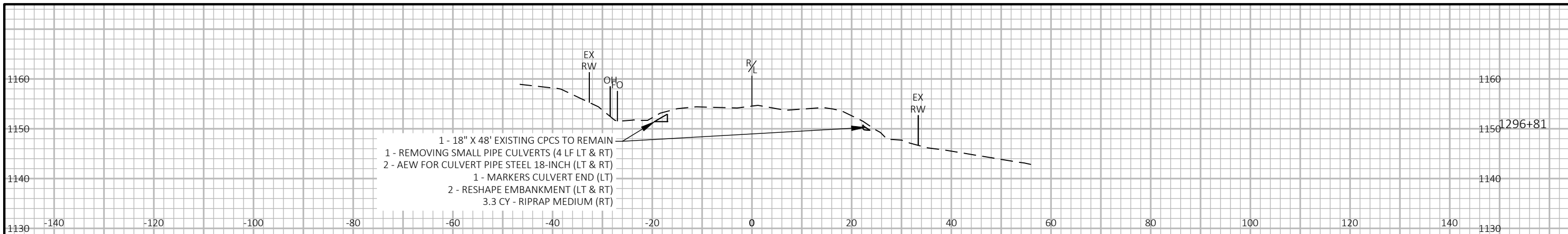


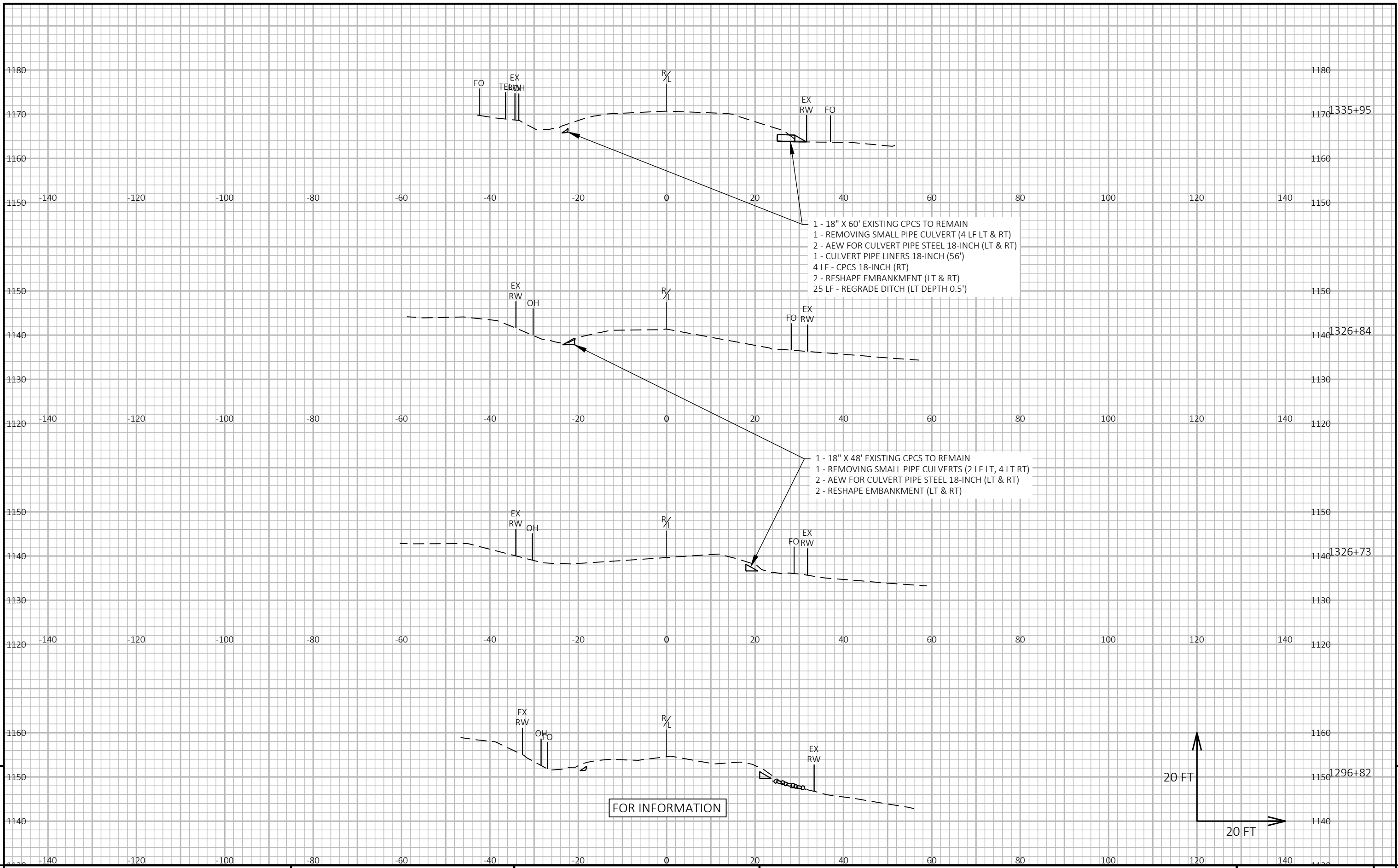
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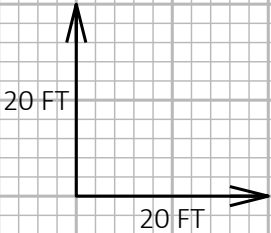
PROJECT NO: 5540-02-71	HWY: STH 27	COUNTY: CRAWFORD	CROSS SECTIONS: STH 27 CULVERT END TREATMENTS	SHEET	E
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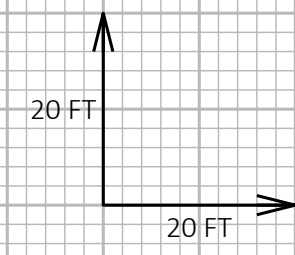
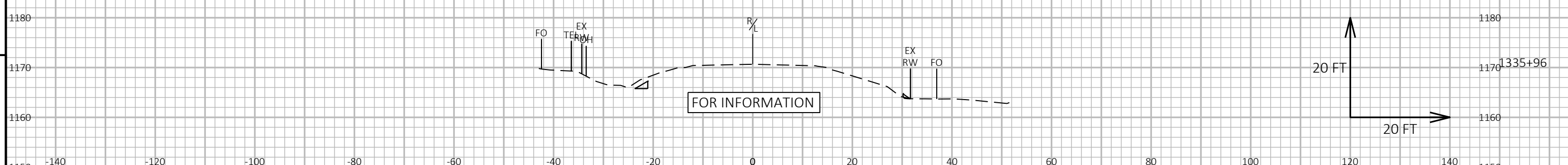
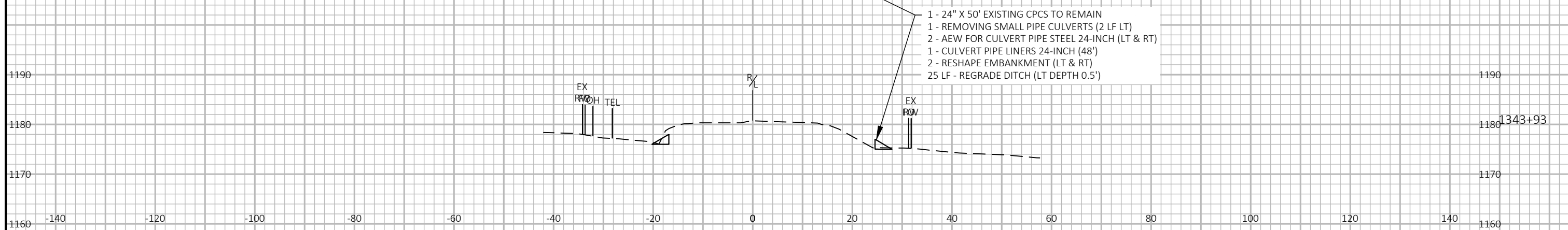
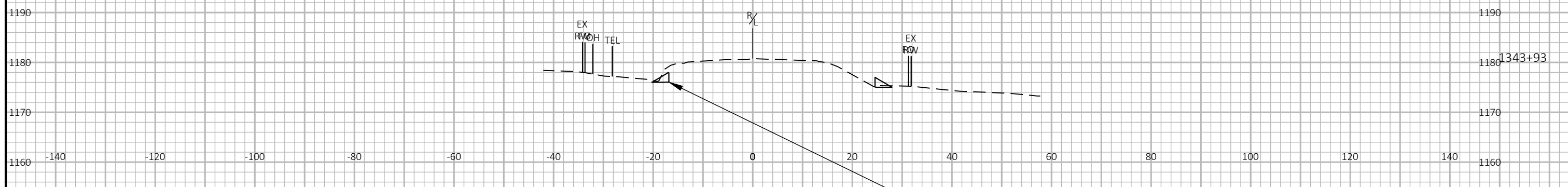
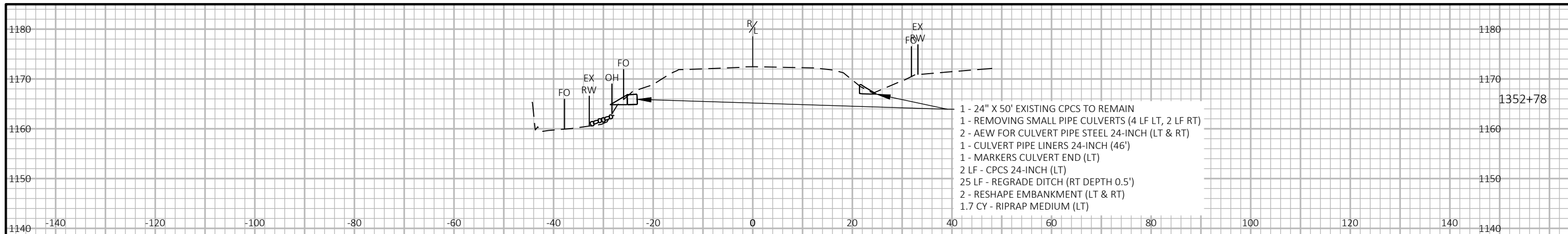


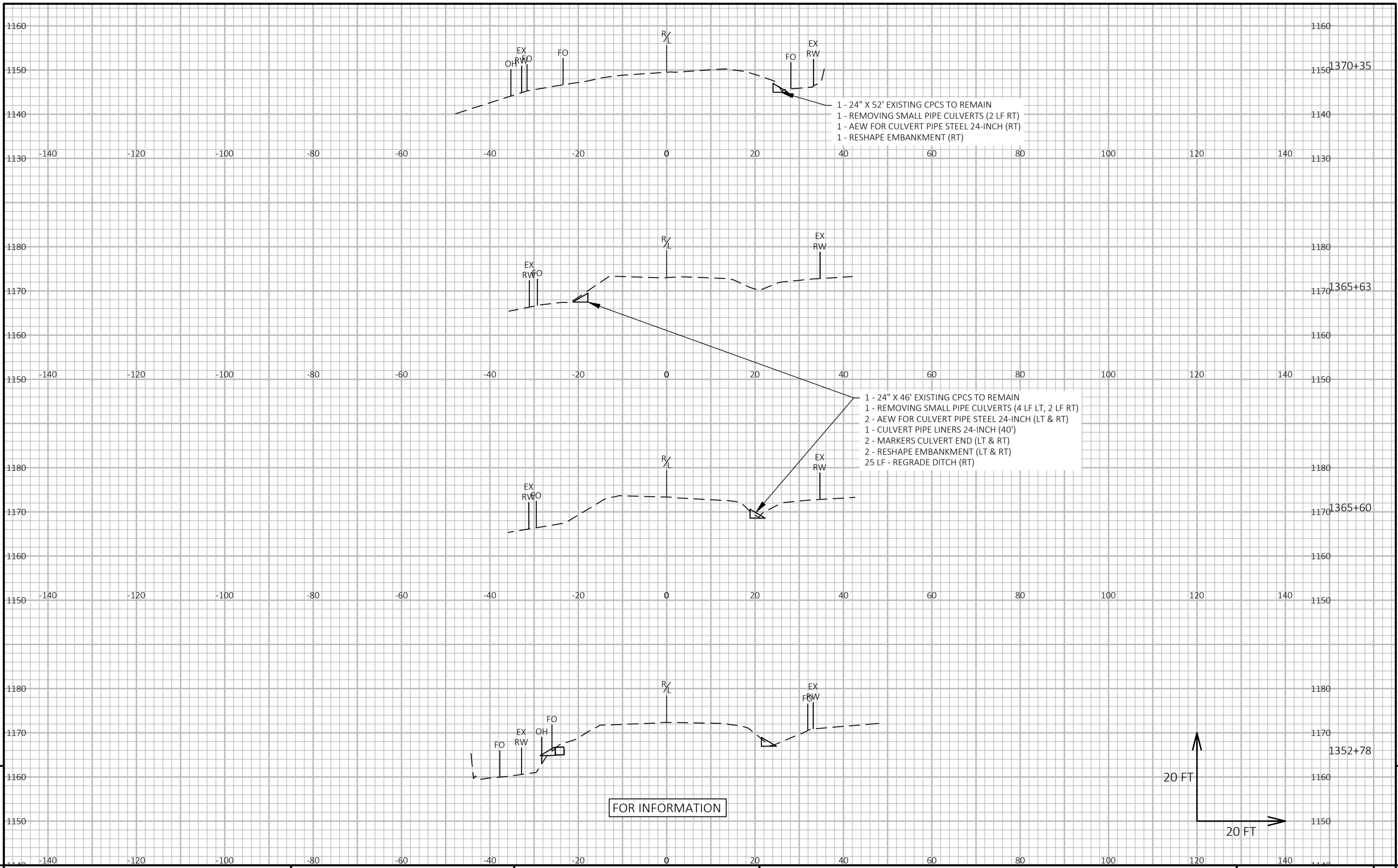




FOR INFORMATION



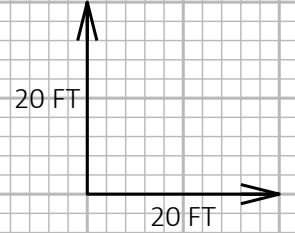


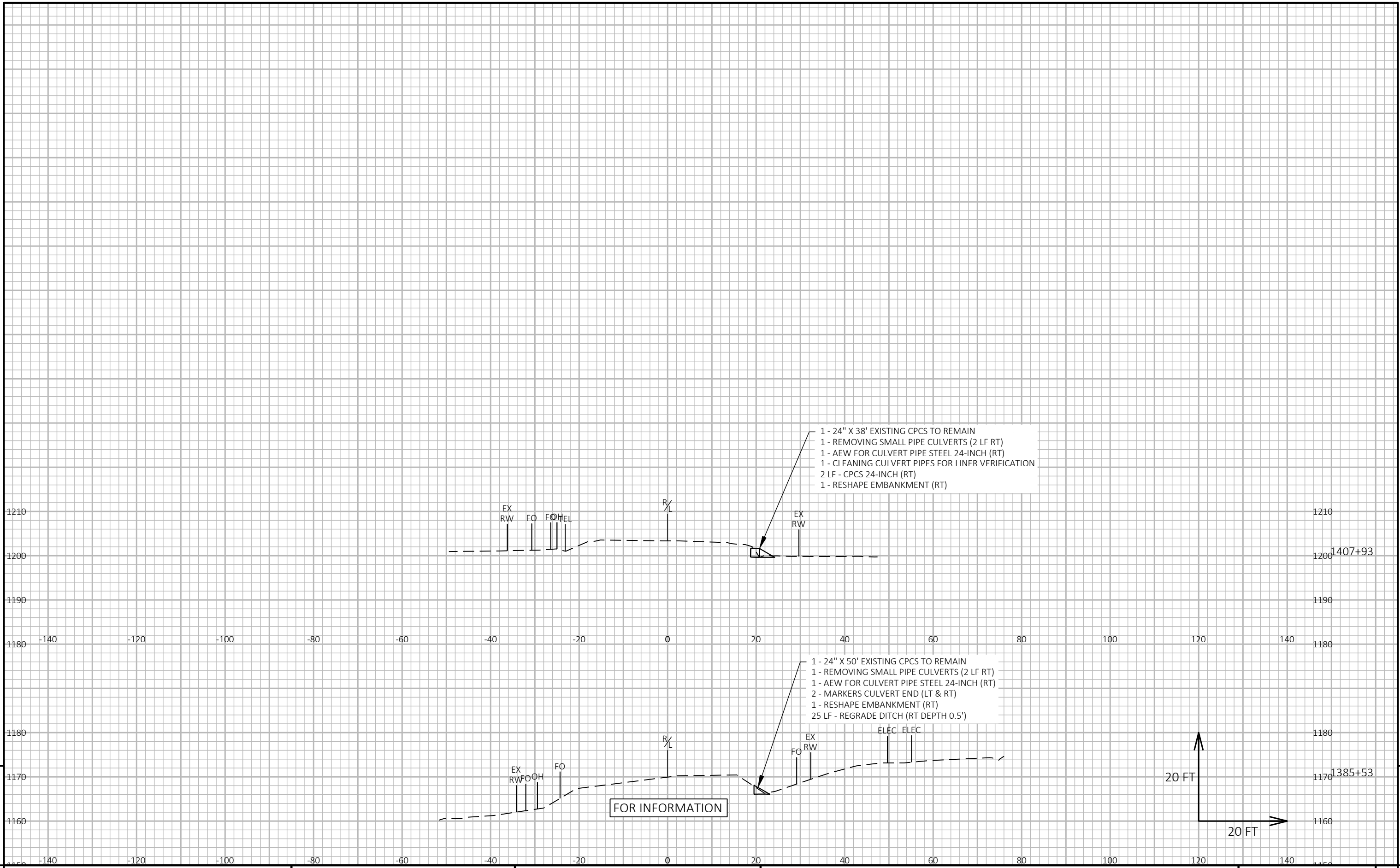


- 1 - 24" X 52' EXISTING CPCS TO REMAIN
- 1 - REMOVING SMALL PIPE CULVERTS (2 LF RT)
- 1 - AEW FOR CULVERT PIPE STEEL 24-INCH (RT)
- 1 - RESHAPE EMBANKMENT (RT)

- 1 - 24" X 46' EXISTING CPCS TO REMAIN
- 1 - REMOVING SMALL PIPE CULVERTS (4 LF LT, 2 LF RT)
- 2 - AEW FOR CULVERT PIPE STEEL 24-INCH (LT & RT)
- 1 - CULVERT PIPE LINERS 24-INCH (40')
- 2 - MARKERS CULVERT END (LT & RT)
- 2 - RESHAPE EMBANKMENT (LT & RT)
- 25 LF - REGRADE DITCH (RT)

FOR INFORMATION





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PROJECT NO: 5540-02-71	HWY: STH 27	COUNTY: CRAWFORD	CROSS SECTIONS: STH 27 CULVERT END TREATMENTS	SHEET	E
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Wisconsin Department of Transportation

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