MAD

MARCH 2024

PROJECT ID: 5310-02-78

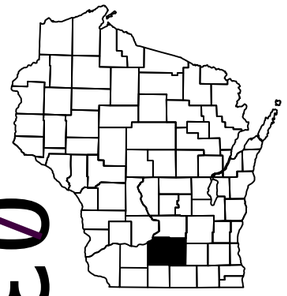
WITH: N/A

COUNTY: DANE COUNTY

ORDER OF SHEETS

Section No.	1	Title
Section No.	2	Typical Sections and Details
Section No.	3	Estimate of Quantities
Section No.	3	Miscellaneous Quantities
~~Section No.~~	~~4~~	~~Right of Way Plat~~
Section No.	5	Plan and Profile
Section No.	6	Standard Detail Drawings
~~Section No.~~	~~7~~	~~Sign Plates~~
~~Section No.~~	~~8~~	~~Structure Plans~~
Section No.	9	Computer Earthwork Data
Section No.	9	Cross Sections

TOTAL SHEETS = 98



03

N

DESIGN DESIGNATION 5310-02-08
LEFT TURN LANES ONLY

A.A.D.T.	2024	=	6800
A.A.D.T.	2044	=	6800
D.H.V.		=	
D.D.		=	
T.		=	7.1 %
DESIGN SPEED		=	45 MPH
ESALS (FLEXIBLE)		=	1,100,000

CONVENTIONAL SYMBOLS

PLAN

- CORPORATE LIMITS
- PROPERTY LINE
- LOT LINE
- LIMITED HIGHWAY EASEMENT
- EXISTING RIGHT OF WAY
- PROPOSED OR NEW R/W LINE
- SLOPE INTERCEPT
- REFERENCE LINE (300'EB')
- EXISTING CULVERT
- PROPOSED CULVERT (Box or Pipe)
- COMBUSTIBLE FLUIDS (CAUTION)
- MARSH AREA
- WOODED OR SHRUB AREA

PROFILE

- GRADE LINE
- ORIGINAL GROUND
- MARSH OR ROCK PROFILE (To be noted as such) (ROCK)
- SPECIAL DITCH (LABEL)
- GRADE ELEVATION (95.36)
- CULVERT (Profile View)

UTILITIES

- ELECTRIC (E)
- FIBER OPTIC (FO)
- GAS (G)
- SANITARY SEWER (SAN)
- STORM SEWER (SS)
- TELEPHONE (T)
- WATER (W)
- UTILITY PEDESTAL
- POWER POLE
- TELEPHONE POLE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

SPRING GREEN - MADISON

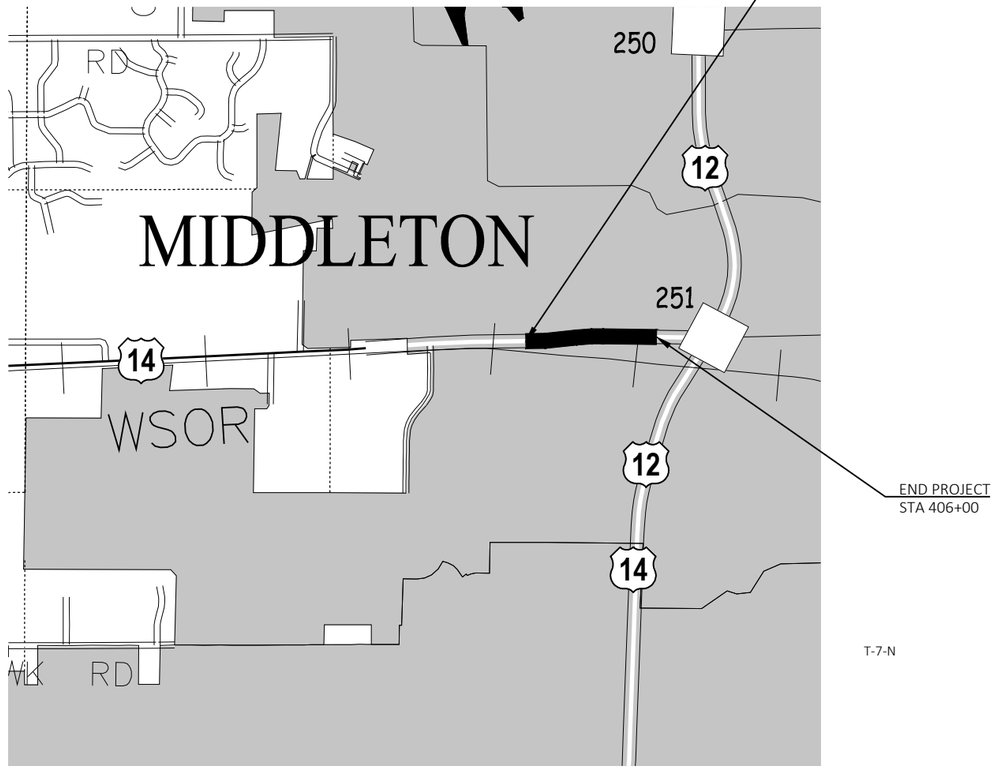
DEMING WAY INTERSECTION

USH 14

DANE COUNTY

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
5310-02-78	WISC 2024273	1

STATE PROJECT NUMBER
5310-02-78



LAYOUT

SCALE 0 0.5 MI

TOTAL NET LENGTH OF CENTERLINE = 0.334 MILES

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS), DANE COUNTY NAD83 (2011), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES.

ELEVATIONS ARE REFERENCED TO NAVD 88 (2012). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 12A

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY

Surveyor	SURVEYOR
Designer	ADAM SHAW
Project Manager	AMY COUGHLIN, P.E
Regional Examiner	REGIONAL EXAMINER
Regional Supervisor	ALEX HAGEN, P.E

APPROVED FOR THE DEPARTMENT

DATE: 10/30/2023 Amy Coughlin (Signature)

E

FILE NAME : \\FIWMAD7P3158\N3PUBLIC\PDS\C3D\53100208\SHEETSPLAN\010101-TI.DWG PLOT DATE : 10/20/2023 11:43 AM PLOT BY : SHAW, ADAM M PLOT NAME :

2

GENERAL NOTES

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

THE CONTRACTOR'S PAVING OPERATIONS SHALL BE CONSISTENT WITH THE PLAN TYPICAL SECTIONS AND CONSTRUCTED TO PREVENT HMA LONGITUDINAL JOINTS FROM BEING LOCATED WITHIN A DRIVING, TURNING, BIKE, OR PARKING LANE.

HMA PAVEMENT WEIGHT CALCULATIONS ARE BASED ON 112 LB/SY/IN.

APPLY TACK COAT AT A RATE OF 0.05 GAL/SY BETWEEN LAYERS OF HMA PAVEMENT.

6 INCH HMA PAVEMENT 4 MT 58-28 H, SHALL BE CONSTRUCTED IN THREE LAYERS WITH 2 INCH UPPER LAYER AND 2 INCH LOWER LAYERS.

CONTRACTOR WILL BE RESPONSIBLE FOR RESHAPING AND SEEDING ANY PREVIOUSLY GRASSED AREAS WHICH ARE DISTURBED BY THEIR OPERATION OUTSIDE OF THE NORMAL CONSTRUCTION LIMITS.

SALVAGED TOPSOIL SHALL BE PLACED 1 INCH BELOW THE TOP OF ADJACENT CONCRETE CURBS OR SIDEWALKS.

THE QUANTITY OF THE ITEMS FOR EROSION PROTECTION INCLUDES AN UNDISTRIBUTED AMOUNT FOR PROTECTION, CONTROL, AND ABATEMENT OF WATER POLLUTION RESULTING FROM SOIL EROSION. THE DISTRIBUTION AND LOCATION OF THESE MATERIALS ARE TO BE DETERMINED BY THE ENGINEER.

DISTURBED AREAS WITHIN THE RIGHT OF WAY ARE TO BE TOPSOILED (SALVAGED), FERTILIZED, SEEDED, AND EROSION MAT PLACED DIRECTED BY THE ENGINEER.

STANDARD ABBREVIATIONS

AC	ACRE	LS	LUMP SUM
AGG	AGGREGATE	MEF	MAXIMUM EXTENT FEASIBLE
<	ANGLE	M.P.	MARKER POST
AE, AEW	APRON ENDWALL	MGAL	1000 GALLONS
ASPH.	ASPHALTIC	N.C.	NORMAL CROWN
A.D.T.	AVERAGE DAILY TRAFFIC	N	NORTH
A.A.D.T.	ANNUAL AVERAGE DAILY TRAFFIC	NB	NORTHBOUND
B.F.	BACK FACE	NOR	NORMAL
BM	BENCHMARK	NO.	NUMBER
BTWN	BETWEEN	PAV'T	PAVEMENT
CTR.	CENTER	P.L.E.	PERMANENT LIMITED EASEMENT
C/L	CENTER LINE	P.C.	POINT OF CURVATURE
Δ	CENTRAL ANGLE OR DELTA	P.I.	POINT OF INTERSECTION
C.E.	COMMERCIAL ENTRANCE	P.T.	POINT OF TANGENCY
CONST.	CONSTRUCTION	PCC	PORTLAND CEMENT CONCRETE
CMCP	CORRUGATED METAL CULVERT PIPE	P.E.	PRIVATE ENTRANCE
CMP	CORRUGATED METAL PIPE	PGL	PROFILE GRADE LINE
CO.	COUNTY	P.L.	PROPERTY LINE
CTH	COUNTY TRUNK HIGHWAY	R	RADIUS OR RANGE
CR.	CREEK	R/L	REFERENCE LINE
CABC	CRUSHED AGGREGATE BASE COURSE	R.C.C.P.	REINFORCED CONCRETE CULVERT PIPE
CY	CUBIC YARD	REQ'D	REQUIRED
CP	CONTROL POINT OR CULVERT PIPE	RT	RIGHT
C&G	CURB AND GUTTER	R.H.F.	RIGHT HAND FORWARD
D	DEGREE OF CURVE	R/W	RIGHT OF WAY
D.H.V.	DESIGN HOURLY VOLUME	RD.	ROAD
DIA.	DIAMETER	SHLD.	SHOULDER(S)
D.D.	DIRECTIONAL DISTRIBUTION	SHR.	SHRINKAGE
DISCH.	DISCHARGE	S	SOUTH
DMS	DYNAMIC MESSAGE SIGN	SB	SOUTHBOUND
EA	EACH	S.F.	SQUARE FOOT (FEET)
E	EAST	SDD	STANDARD DETAIL DRAWING(S)
EB	EASTBOUND	STH	STATE TRUNK HIGHWAY
ELEC.	ELECTRIC(AL), ELEC. CABLE	STA.	STATION
EL., ELEV.	ELEVATION	S.E.	SUPERELEVATION
ESALS	EQUIVALENT SINGLE AXLE LOADS	S/L	SURVEY LINE
EXC.	EXCAVATION	SYM	SYMMETRICAL
EXIST	EXISTING	T.	PERCENT TRUCKS
F.F.	FACE TO FACE	TEL.	TELEPHONE
FERT.	FERTILIZER	TEMP.	TEMPORARY
F.E.	FIELD ENTRANCE	T.L.E.	TEMPORARY LIMITED EASEMENT
F/L, F.L.	FLOW LINE	T.O.C.	TOP OF CURB
GALV.	GALVANIZE	TYP	TYPICAL
H.S.	HIGH STRENGTH	UNCL	UNCLASSIFIED
CWT	HUNDRED WEIGHT	U.H.	UNDERGROUND (CABLE)
INL	INLET	VAR	VARIABLE
INTER.	INTERSECTION	V.C.	VERTICAL CURVE
IH	INTERSTATE HIGHWAY	V.P.C.	VERTICAL POINT OF CURVATURE
JT.	JOINT	V.P.I.	VERTICAL POINT OF INTERSECTION
LT	LEFT	V.P.T.	VERTICAL POINT OF TANGENCY
L.H.F.	LEFT HAND FORWARD	Wt.	WEIGHT
L	LENGTH OF CURVE	W	WEST
L.F.	LINEAR FOOT (FEET)	WB	WESTBOUND
LC	LONG CHORD		

DESIGN PROJECT LEADER

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ALLIANT ENERGY - COMMUNICATION
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CEDAR RAPIDS, IA 52401
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EMAIL: LAURASEALS@ALLIANTENERGY.COM

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4955 BULLIS FARM RD
EAU CLAIRIE, WI 53701
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RAHLES@MGE.COM

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DIGGERS HOTLINE
Dial 811 or (800)242-8511
www.DiggersHotline.com

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2

CITY OF MIDDLETON

N

MURPHY DRIVE

DEMING WAY

"D" LINE

EAGLE DRIVE

PHEASANT BRANCH

USH 14

UNIVERSITY AVENUE

UNIVERSITY AVENUE

USH 14

DEMING WAY

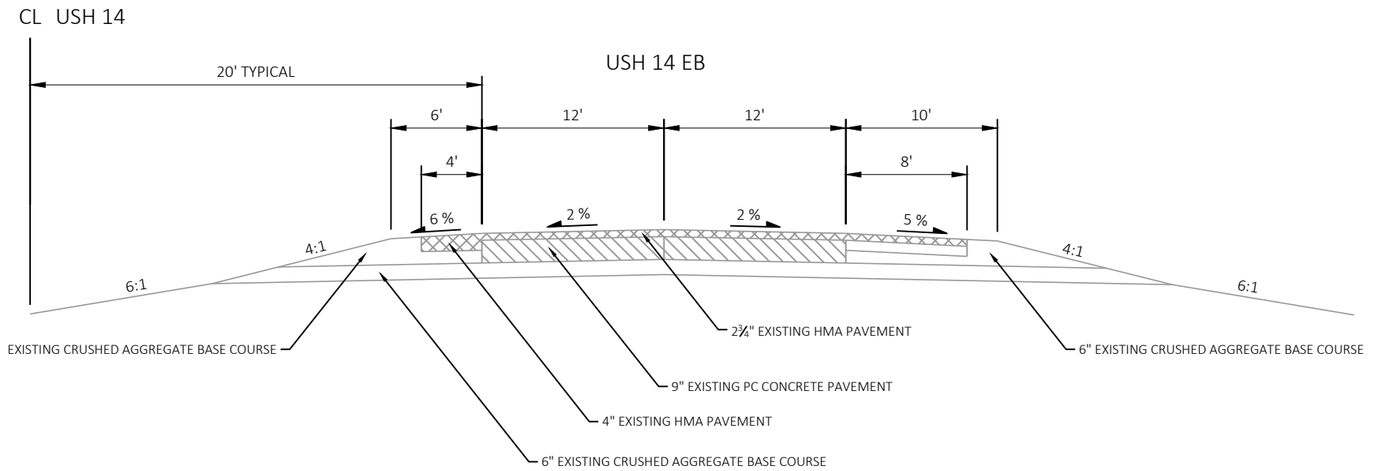
BEGIN PROJECT
STA 388+35

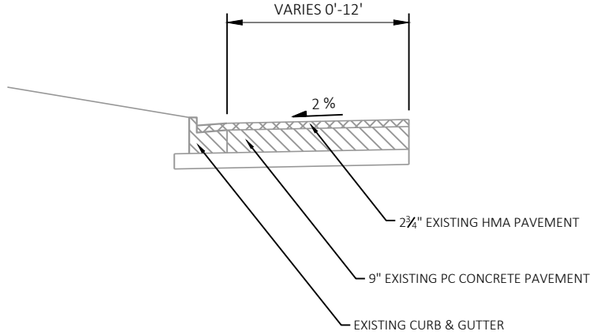
TERRACE AVENUE

END PROJECT
STA 406+00

PROJECT NO: 5310-02-78	HWY: USH 14	COUNTY: DANE	PROJECT OVERVIEW	SHEET 3	E

2





EXISTING LEFT TURN TYPICAL SECTION
USH 14 EASTBOUND
STA 391+55 TO 392+50

EXISTING TYPICAL SECTION
USH 14 EASTBOUND
STA 388+00 TO STA 392+50

PROJECT NO: 5310-02-78	HWY: USH 14	COUNTY: DANE	TYPICAL SECTIONS	SHEET 4	E

FILE NAME : N:\PDS\C3D\53100208\SHEETSPLAN\020301-TS.DWG
LAYOUT NAME - 01-10ft
PLOT DATE : 10/31/2023 8:02 AM
PLOT BY : SHAW, ADAM M
PLOT NAME :
PLOT SCALE : 1 IN:10 FT

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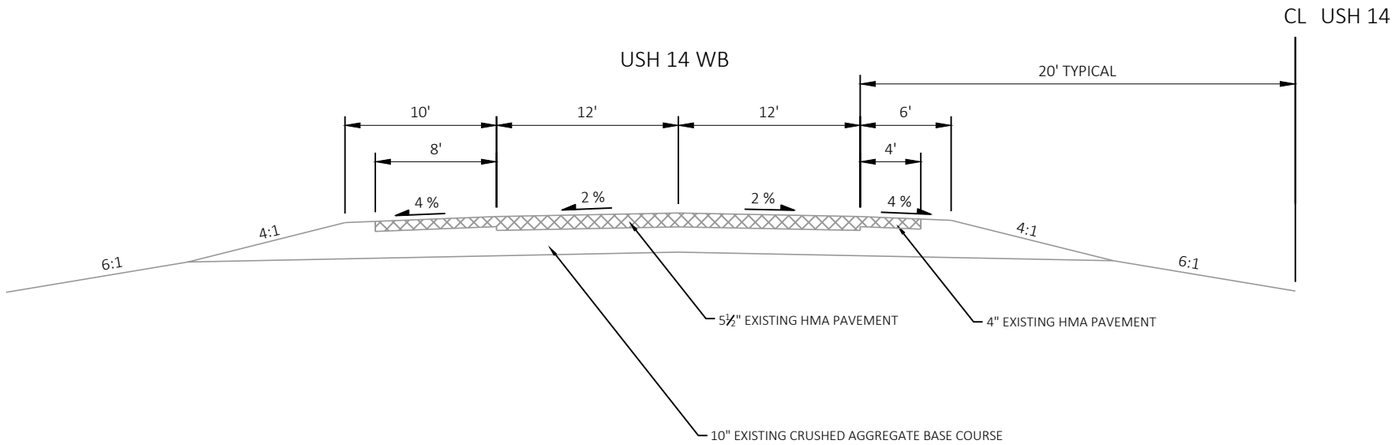
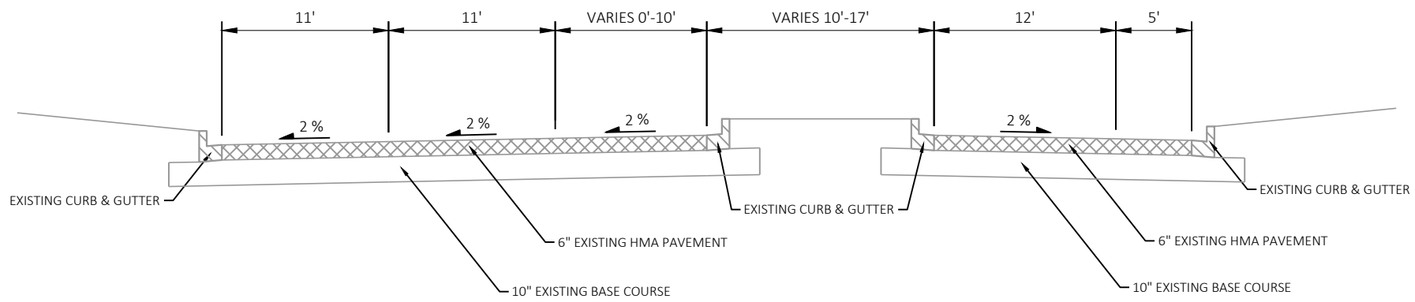


EXISTING TYPICAL SECTION
USH 14 WESTBOUND
STA 401+00 TO STA 406+00



EXISTING TYPICAL SECTION
DEMING WAY
STA 2+95 'D' TO STA 4+35 'D'

PROJECT NO: 5310-02-78	HWY: USH 14	COUNTY: DANE	TYPICAL SECTIONS	SHEET 5	E

FILE NAME : N:\PDS\C3D\53100208\SHEETSPLAN\020301-TS.DWG
LAYOUT NAME - 02-10ft
PLOT DATE : 10/31/2023 8:02 AM
PLOT BY : SHAW, ADAM M
PLOT NAME :
PLOT SCALE : 1 IN:10 FT
WISDOT/CADDS SHEET 42

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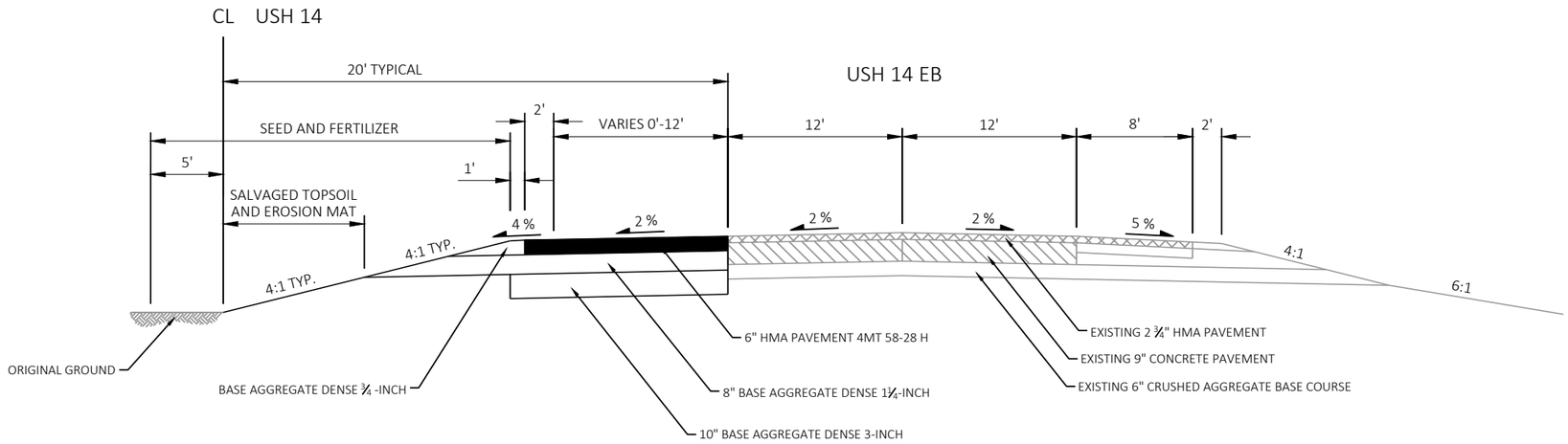
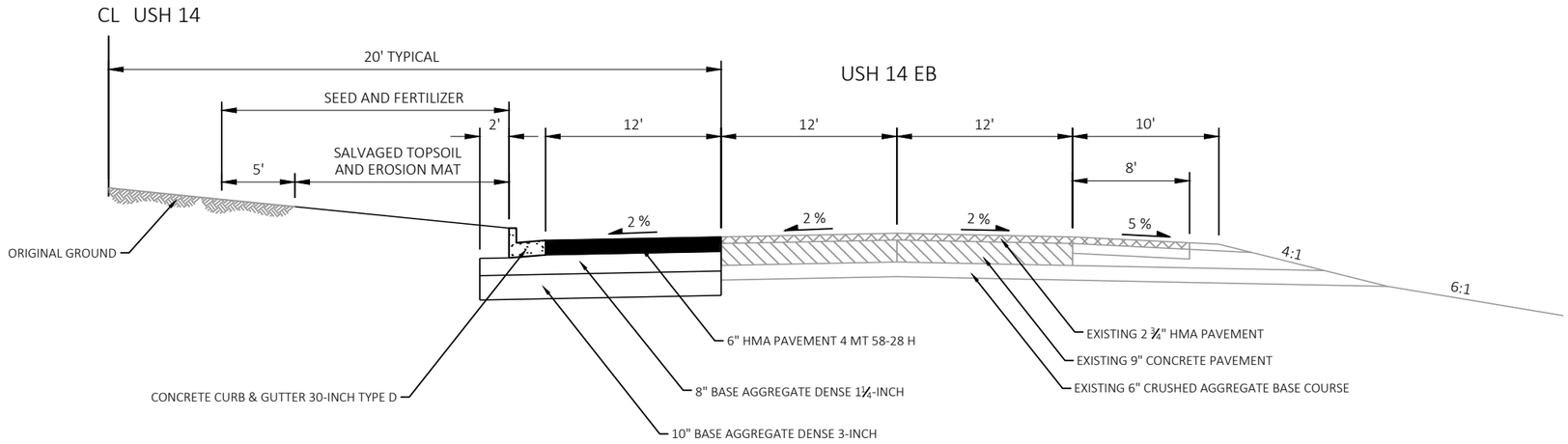


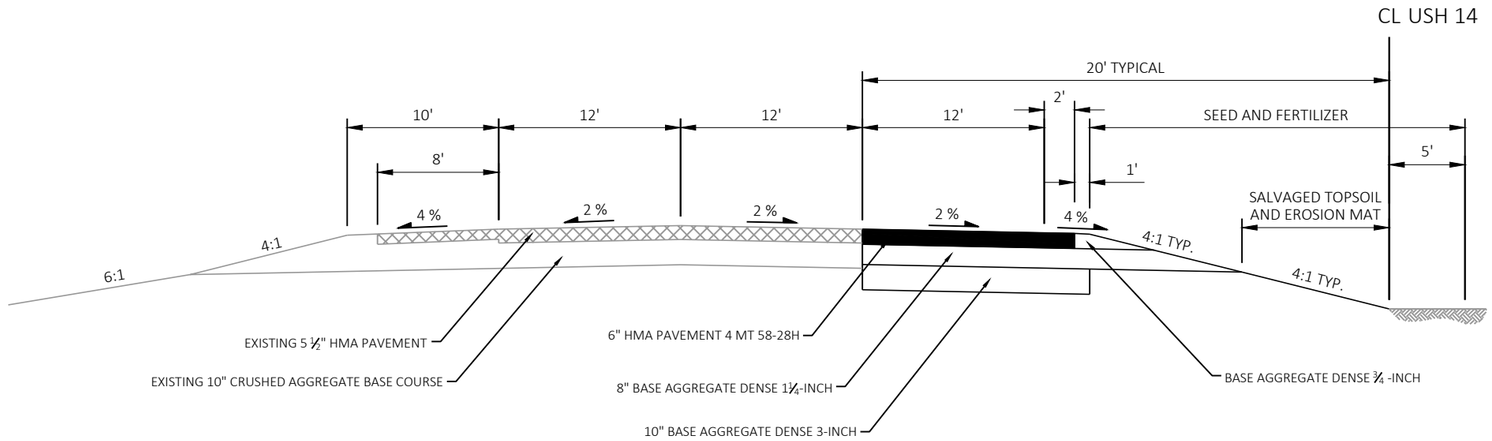
FINISHED TYPICAL SECTION
USH 14 EASTBOUND
STA 388+35 TO STA 391+00



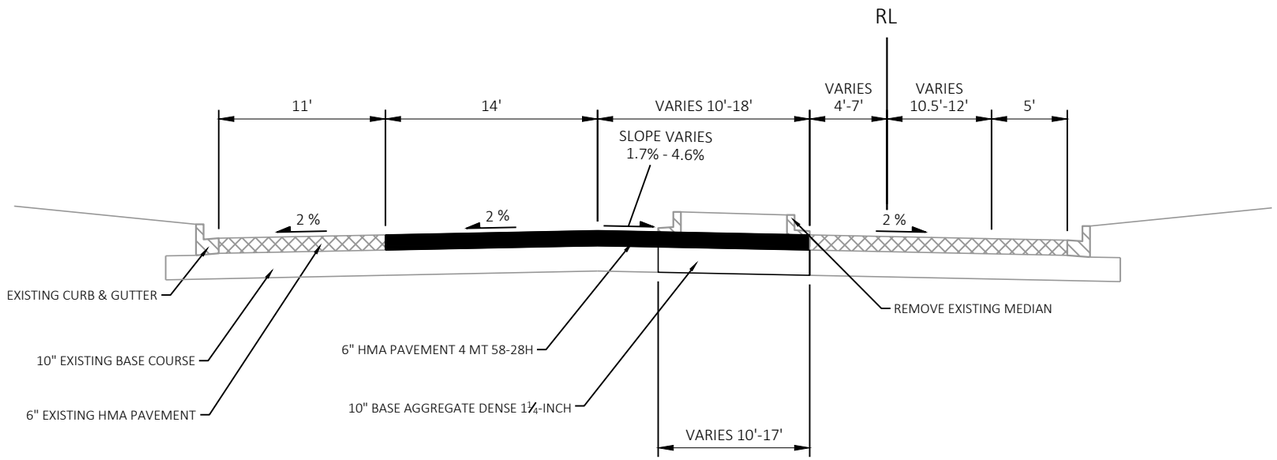
FINISHED TYPICAL SECTION
USH 14 EASTBOUND
STA 391+00 TO STA 392+50

PROJECT NO: 5310-02-78	HWY: USH 14	COUNTY: DANE	TYPICAL SECTIONS	SHEET 6	E

2



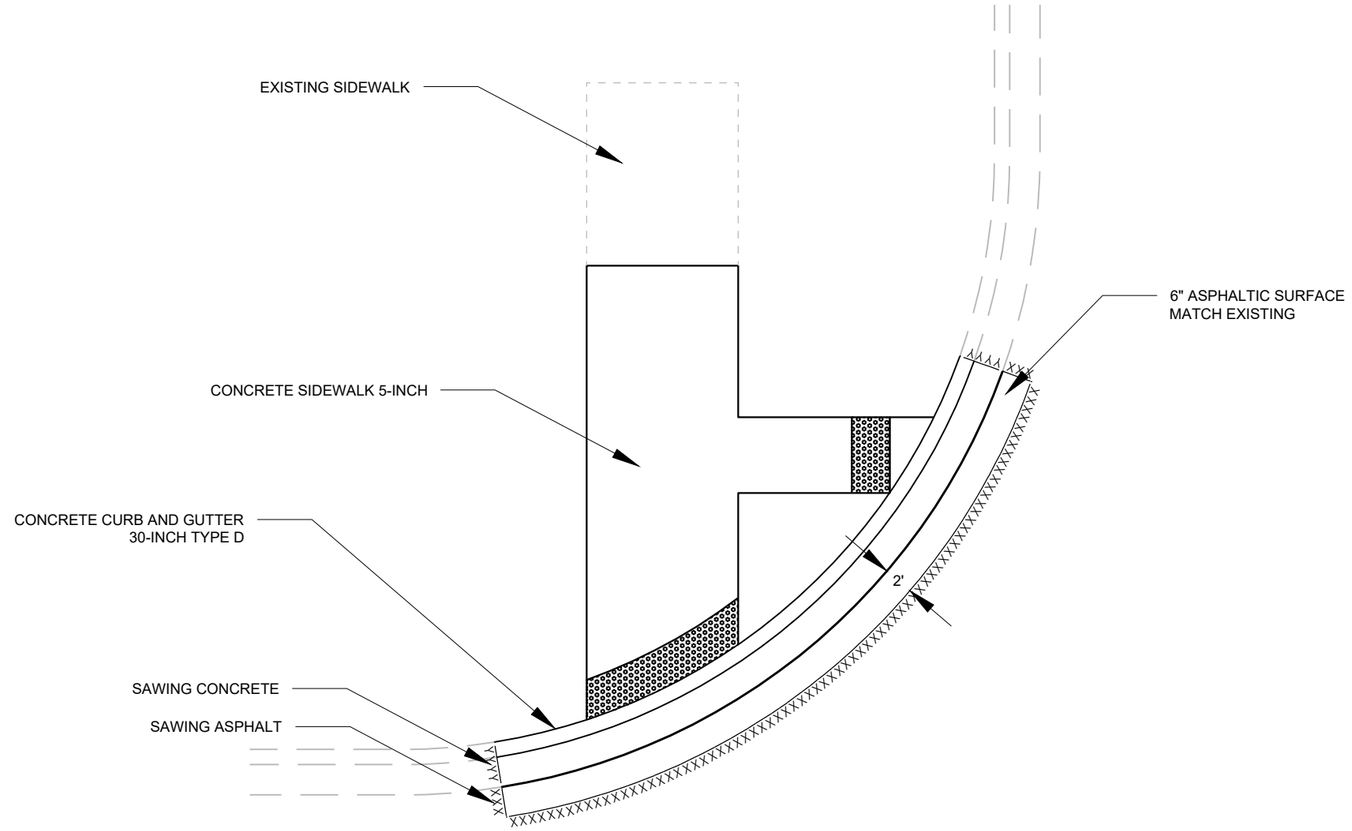
FINISHED TYPICAL SECTION
USH 14 WESTBOUND
STA 401+00 TO STA 406+00



PROPOSED TYPICAL SECTION
DEMING WAY
STA 2+95 'D' TO STA 4+35 'D'

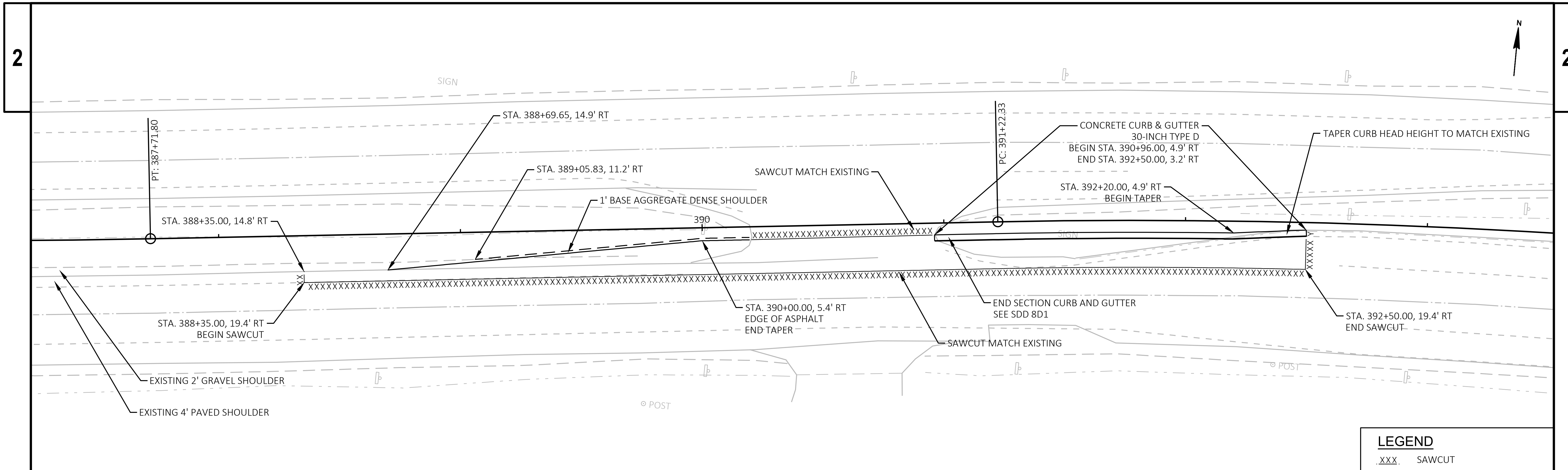
PROJECT NO: 5310-02-78	HWY: USH 14	COUNTY: DANE	TYPICAL SECTIONS	SHEET 7	E

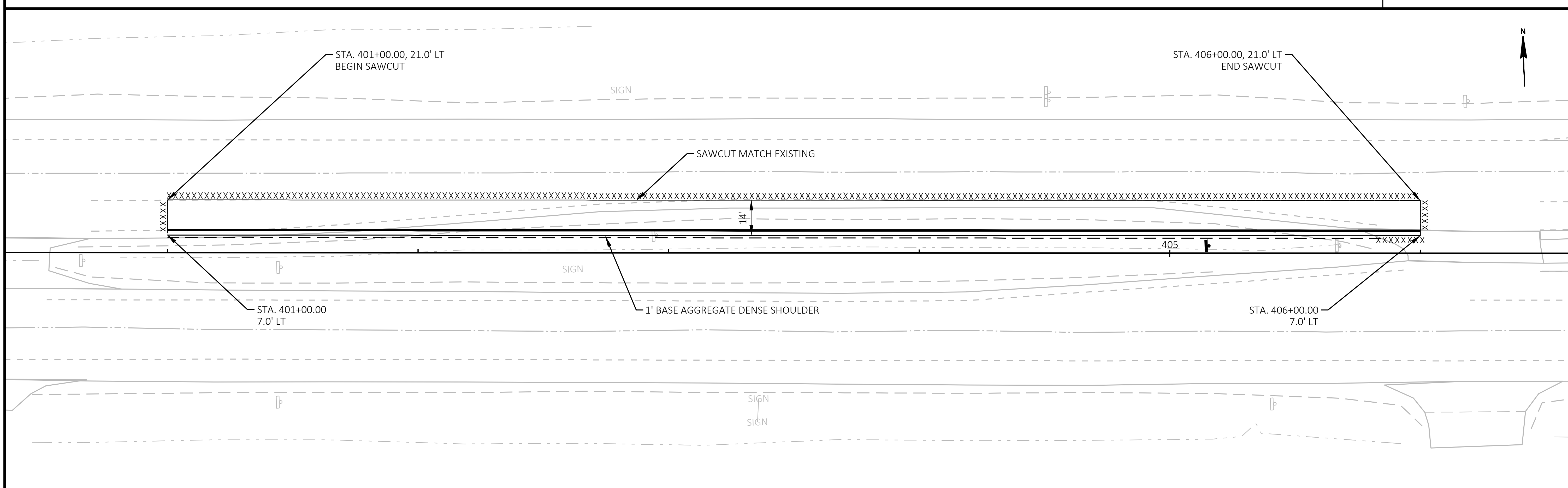
2



ASPHALT REMOVAL AT CURB RAMPS

PROJECT NO: 5310-02-08	HWY: USH 14	COUNTY: DANE	CONSTRUCTION DETAIL	SHEET 8	E

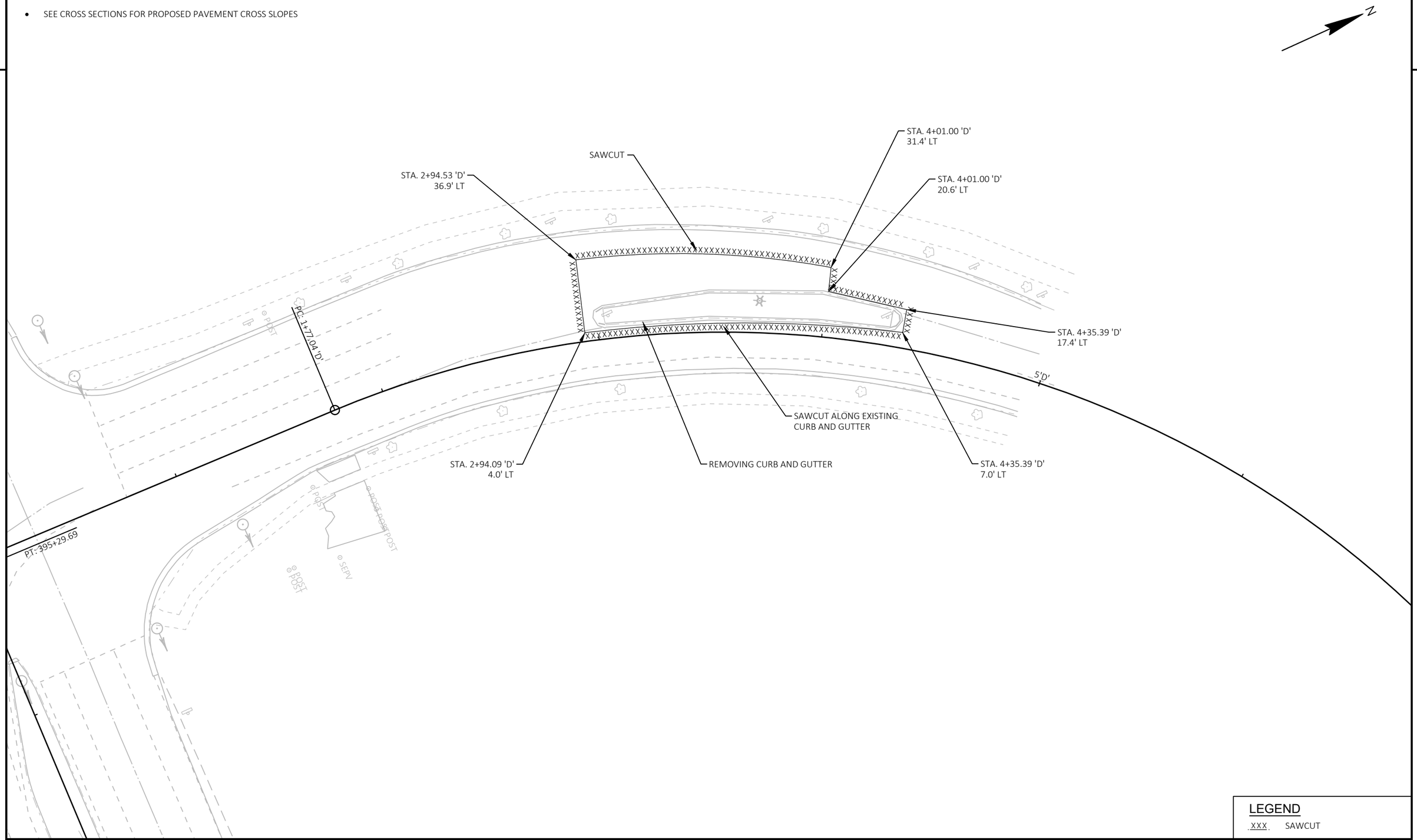




PROJECT NO: 5310-02-78	HWY: USH 14	COUNTY: DANE	CONSTRUCTION DETAILS	SHEET 9	E

- SIGNS AND LIGHT POLE IN DEMING WAY MEDIAN WILL BE REMOVED BY OTHERS
- SEE CROSS SECTIONS FOR PROPOSED PAVEMENT CROSS SLOPES

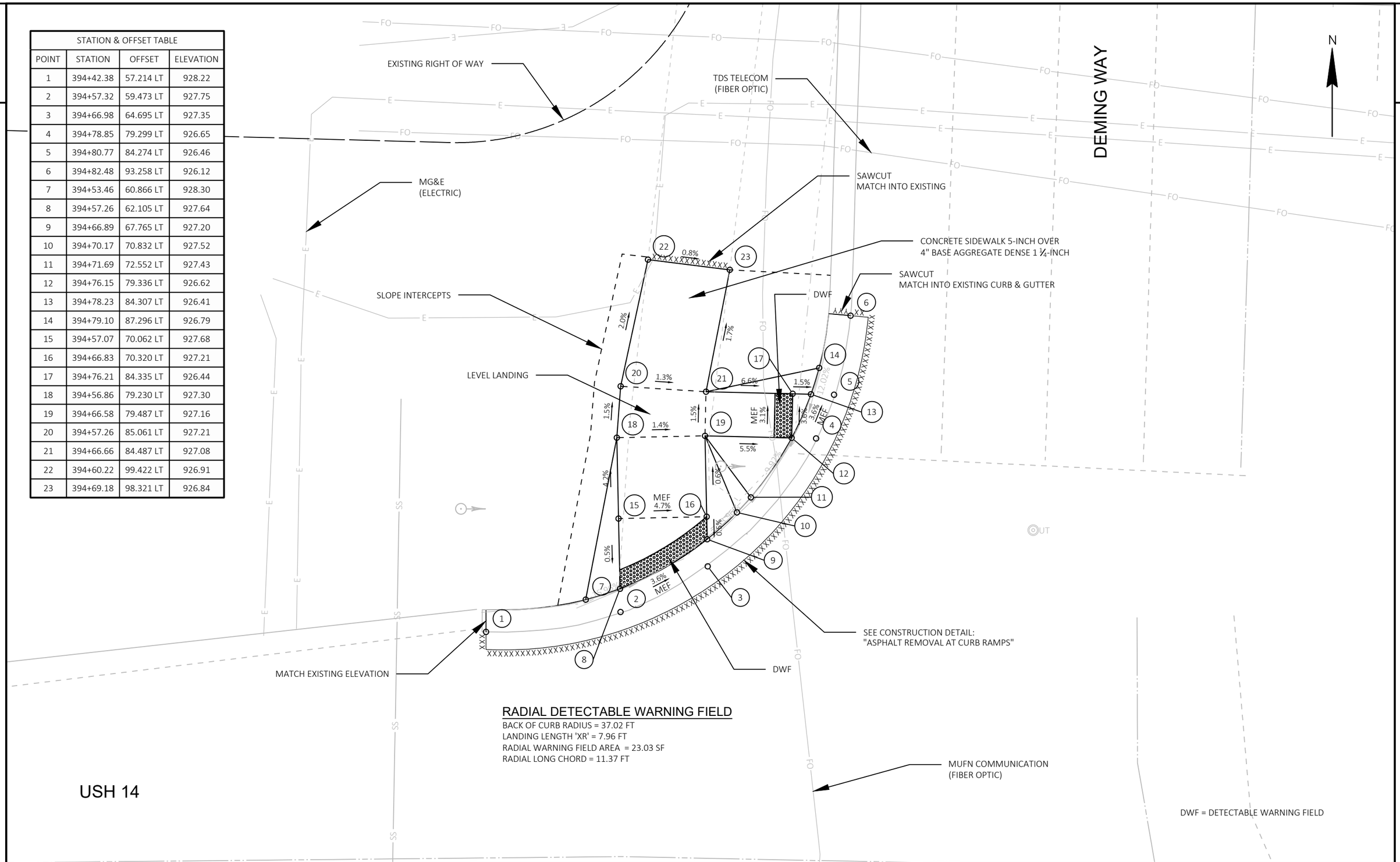
2



PROJECT NO: 5310-02-78	HWY: USH 14	COUNTY: DANE	CONSTRUCTION DETAILS	SHEET 10	E

FILE NAME : N:\PDS\C3D\53100208\SHEETSPLAN\021002-CD.DWG
LAYOUT NAME - 07
PLOT DATE : 10/31/2023 8:02 AM
PLOT BY : SHAW, ADAM M
PLOT NAME :
PLOT SCALE : 1 IN:40 FT
WISDOT/CADDS SHEET 42

STATION & OFFSET TABLE			
POINT	STATION	OFFSET	ELEVATION
1	394+42.38	57.214 LT	928.22
2	394+57.32	59.473 LT	927.75
3	394+66.98	64.695 LT	927.35
4	394+78.85	79.299 LT	926.65
5	394+80.77	84.274 LT	926.46
6	394+82.48	93.258 LT	926.12
7	394+53.46	60.866 LT	928.30
8	394+57.26	62.105 LT	927.64
9	394+66.89	67.765 LT	927.20
10	394+70.17	70.832 LT	927.52
11	394+71.69	72.552 LT	927.43
12	394+76.15	79.336 LT	926.62
13	394+78.23	84.307 LT	926.41
14	394+79.10	87.296 LT	926.79
15	394+57.07	70.062 LT	927.68
16	394+66.83	70.320 LT	927.21
17	394+76.21	84.335 LT	926.44
18	394+56.86	79.230 LT	927.30
19	394+66.58	79.487 LT	927.16
20	394+57.26	85.061 LT	927.21
21	394+66.66	84.487 LT	927.08
22	394+60.22	99.422 LT	926.91
23	394+69.18	98.321 LT	926.84



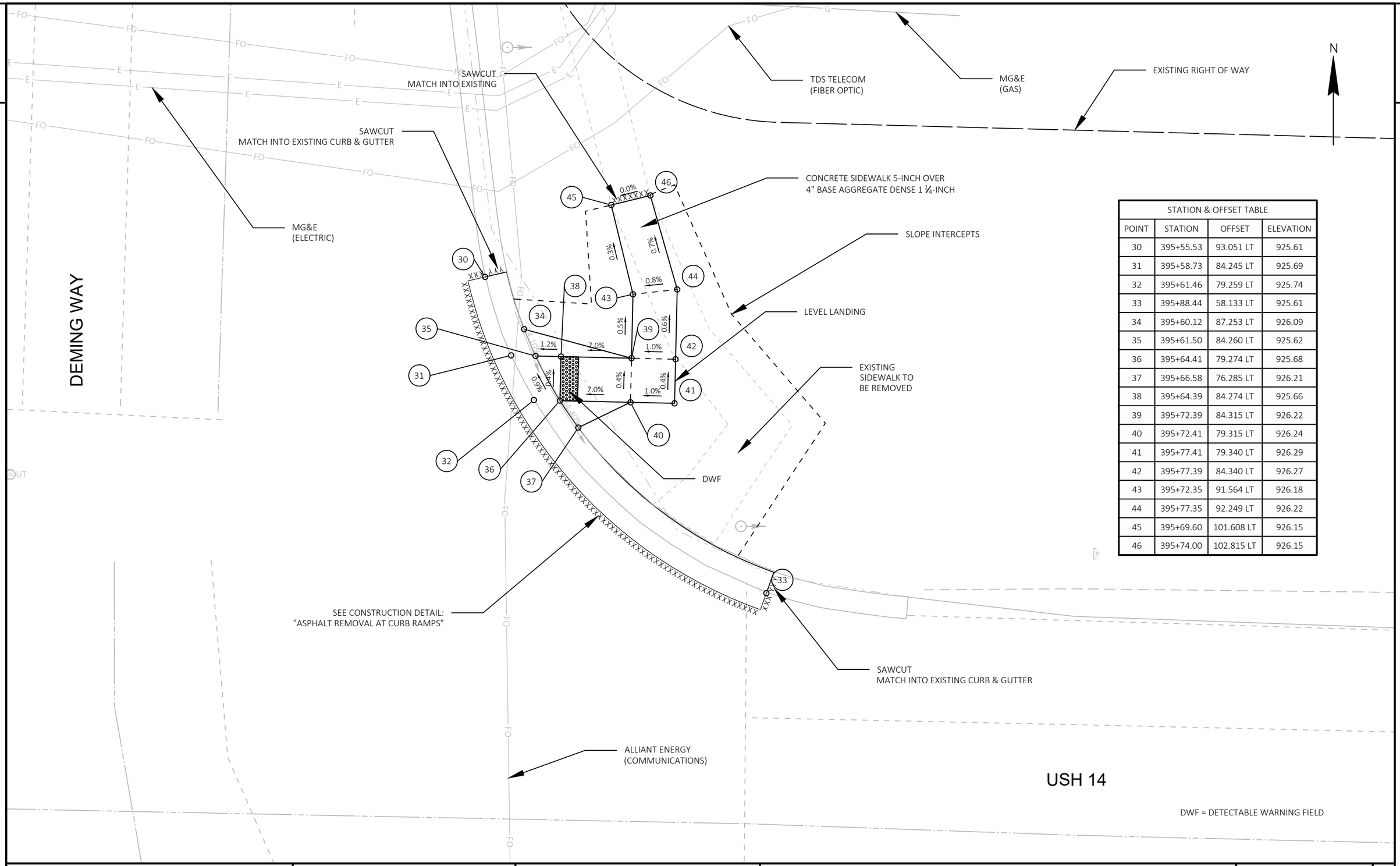
RADIAL DETECTABLE WARNING FIELD

BACK OF CURB RADIUS = 37.02 FT
LANDING LENGTH 'XR' = 7.96 FT
RADIAL WARNING FIELD AREA = 23.03 SF
RADIAL LONG CHORD = 11.37 FT

PROJECT NO: 5310-02-78	HWY: USH 14	COUNTY: DANE	CURB RAMP DETAIL - NORTHWEST	SHEET 11	E

FILE NAME : N:\PDS\C3D\53100208\SHEETSPLAN\021301-CR REVERSE.DWG
LAYOUT NAME - 01
PLOT DATE : 10/31/2023 8:02 AM
PLOT BY : SHAW, ADAM M
PLOT NAME :
PLOT SCALE : 1 IN:10 FT

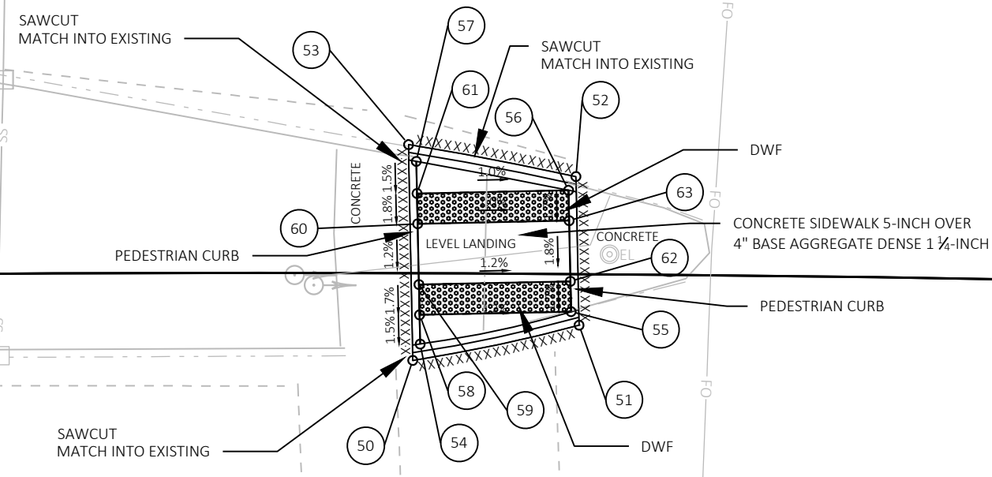
2



STATION & OFFSET TABLE			
POINT	STATION	OFFSET	ELEVATION
30	395+55.53	93.051 LT	925.61
31	395+58.73	84.245 LT	925.69
32	395+61.46	79.259 LT	925.74
33	395+88.44	58.133 LT	925.61
34	395+60.12	87.253 LT	926.09
35	395+61.50	84.260 LT	925.62
36	395+64.41	79.274 LT	925.68
37	395+66.58	76.285 LT	926.21
38	395+64.39	84.274 LT	925.66
39	395+72.39	84.315 LT	926.22
40	395+72.41	79.315 LT	926.24
41	395+77.41	79.340 LT	926.29
42	395+77.39	84.340 LT	926.27
43	395+72.35	91.564 LT	926.18
44	395+77.35	92.249 LT	926.22
45	395+69.60	101.608 LT	926.15
46	395+74.00	102.815 LT	926.15

PROJECT NO: 5310-02-78	HWY: USH 14	COUNTY: DANE	CURB RAMP DETAIL - NORTHEAST	SHEET 12	E

USH 14 WB



USH 14 EB

STATION & OFFSET TABLE			
POINT	STATION	OFFSET	ELEVATION
50	394+58.39	5.755 RT	927.73
51	394+69.37	3.373 RT	927.73
52	394+69.09	6.431 LT	927.28
53	394+58.06	8.482 LT	927.91
54	394+58.87	4.676 RT	927.23
55	394+68.83	2.482 RT	927.14
56	394+68.60	5.521 LT	927.28
57	394+58.57	7.377 LT	927.41
58	394+58.82	2.746 RT	927.26
59	394+58.77	0.747 RT	927.30
60	394+58.68	3.258 LT	927.34
61	394+58.63	5.258 LT	927.38
62	394+68.77	0.483 RT	927.17
63	394+68.66	3.522 LT	927.38

DWF = DETECTABLE WARNING FIELD

PROJECT NO: 5310-02-78	HWY: USH 14	COUNTY: DANE	CURB RAMP DETAIL - MEDIAN	SHEET 13	E

2

USH 14

2

MUFN COMMUNICATION
(FIBER OPTIC)

MIDDLETON MUNICIPAL WATER UTILITY
(WATER)

N

SAWCUT
MATCH INTO EXISTING CURB & GUTTER

SEE CONSTRUCTION DETAIL:
"ASPHALT REMOVAL AT CURB RAMPS"

CAUTION

MG&E
(GAS)

DWF

EXISTING RIGHT OF WAY

LEVEL LANDING

MG&E
(ELECTRIC)

SAWCUT
MATCH INTO EXISTING CURB & GUTTER

SPECTRUM COMMUNICATIONS
(TELEPHONE)

MG&E
(OVERHEAD ELECTRIC)

SPECTRUM COMMUNICATIONS
(FIBER OPTIC)

SLOPE INTERCEPTS

CONCRETE SIDEWALK 5-INCH OVER
4" BASE AGGREGATE DENSE 1 ¼-INCH

SAWCUT
MATCH INTO EXISTING

MATCH
EXISTING
ELEVATIONS

MIDDLETON MUNICIPAL WATER UTILITY
(SANITARY SEWER)

EXISTING RIGHT OF WAY

PERMANENT EASEMENT

DEMING WAY

DWF = DETECTABLE WARNING FIELD

STATION & OFFSET TABLE			
POINT	STATION	OFFSET	ELEVATION
70	394+80.99	76.452 RT	926.23
71	394+70.70	67.300 RT	926.32
72	394+60.30	61.315 RT	926.49
73	394+46.64	56.871 RT	926.66
74	394+73.95	73.073 RT	926.65
75	394+70.80	70.498 RT	926.21
76	394+60.37	64.119 RT	926.39
77	394+56.82	62.546 RT	926.95
78	394+70.88	73.126 RT	926.25
79	394+60.61	73.397 RT	926.44
80	394+71.05	79.123 RT	926.67
81	394+60.77	79.395 RT	926.86
82	394+71.20	84.121 RT	926.74
83	394+60.90	84.394 RT	926.93
84	394+71.87	92.002 RT	927.45
85	394+61.90	92.744 RT	927.59

RADIAL DETECTABLE WARNING FIELD

BACK OF CURB RADIUS = 59.14 FT
LANDING LENGTH 'XR' = 9.28 FT
RADIAL WARNING FIELD AREA = 24.20 SF
RADIAL LONG CHORD = 12.01 FT

PROJECT NO: 5310-02-78	HWY: USH 14	COUNTY: DANE	CURB RAMP DETAIL - SOUTHWEST	SHEET 14	E

2

2

N

SIGN

PT: 387+71.80

PC: 391+22.33

MOVING SIGNS TYPE II

390

12'

STA. 388+20
OFFSET = 19.4' RT

BEGIN TAPER
STA. 388+35
OFFSET = 19.4' RT

MARKING LINE EPOXY 6-INCH YELLOW

END TAPER
STA. 390+00
OFFSET = 7.4' RT

MARKING LINE EPOXY 10-INCH WHITE

POST

MARKING LINE EPOXY 6-INCH YELLOW

N

MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH

PT: 395+29.69

PC: 0+00.00

395

SIGN

12'

STA. 392+50
OFFSET = 5.6' RT

MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH

MATCH INTO EXISTING PAVEMENT MARKING

MARKING LINE EPOXY 10-INCH WHITE

POST

MARKING LINE EPOXY 6-INCH YELLOW

POST

POST

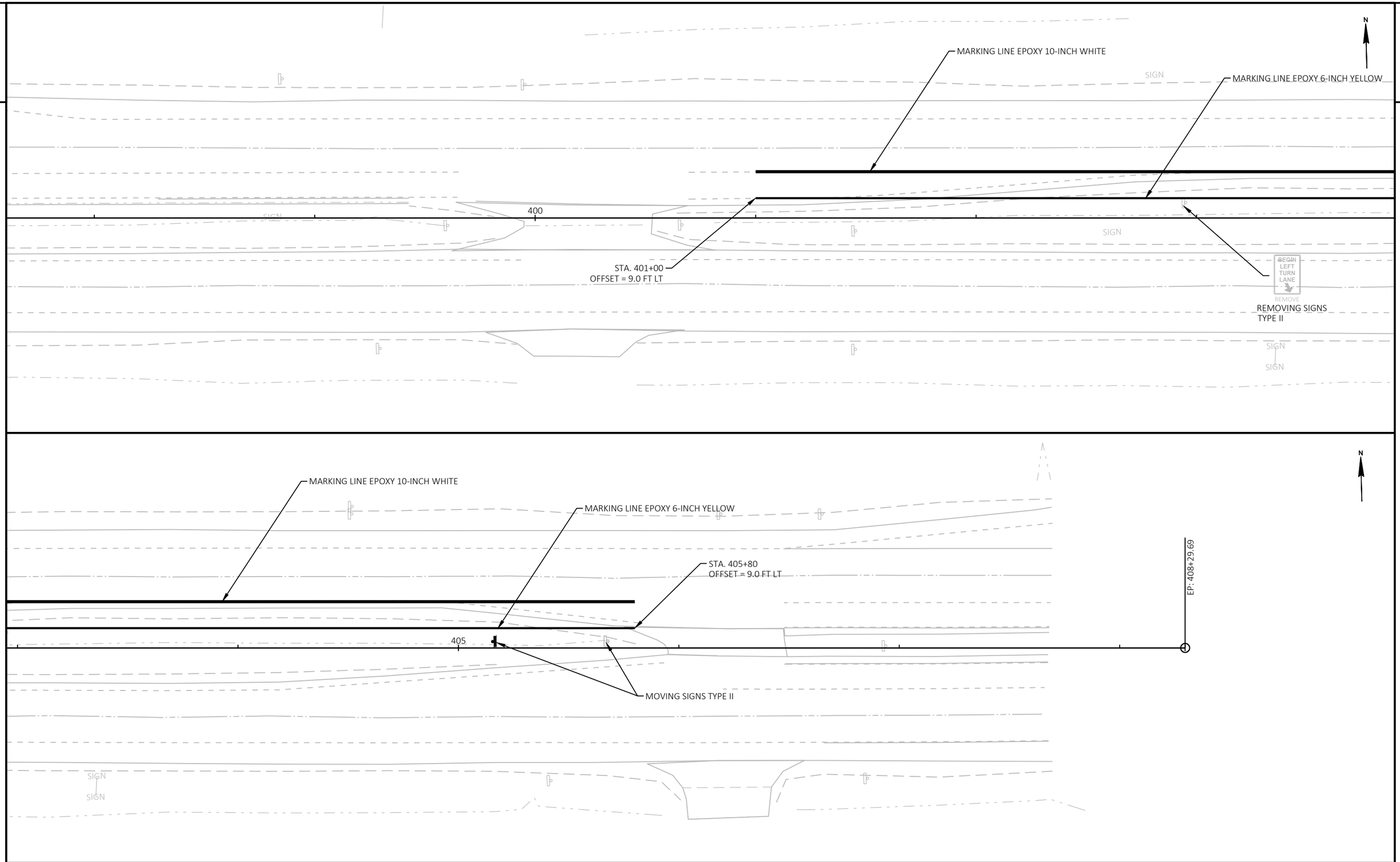
POST

POST

PROJECT NO: 5310-02-78	HWY: USH 14	COUNTY: DANE	PAVEMENT MARKING	SHEET 15	E

FILE NAME : N:\PDS\C3D\53100208\SHEETSPLAN\024501-PM.DWG
LAYOUT NAME - 03

PLOT DATE : 10/31/2023 2:58 PM PLOT BY : SHAW, ADAM M PLOT NAME : PLOT SCALE : 1 IN:40 FT



2

PROJECT NO: 5310-02-78	HWY: USH 14	COUNTY: DANE	PAVEMENT MARKING	SHEET 16	E

2

N

MARKING LINE PAINT 4-INCH WHITE
10 LF SKIPS, 30 LF GAPS

MARKING LINE PAINT 4-INCH WHITE
2 LF SKIPS, 4 LF GAPS

STA. 4+35 'D'
END TAPER

MATCH INTO EXISTING
PAVEMENT MARKING

MARKING LINE PAINT 8-INCH WHITE
MATCH INTO EXISTING PAVEMENT MARKING

MARKING STOP LINE
PAINT 18-INCH

11'
11'
11'

PC: 1+77.04 'D'

4'

6'

5'D'

BEGIN TAPER
STA. 3+50 'D'

MATCH INTO EXISTING
PAVEMENT MARKING

MARKING LINE PAINT 4-INCH
DOUBLE YELLOW

MARKING LINE PAINT 8-INCH WHITE

PT: 395+29.69

MARKING REMOVAL LINE
4-INCH AND MARKING
REMOVAL LINE 6-INCH TO
NEW STOP BAR LOCATION

MARKING CROSSWALK PAINT TRANSVERSE LINE 6-INCH

PROJECT NO: 5310-02-78	HWY: USH 14	COUNTY: DANE	PAVEMENT MARKING	SHEET 17	E

FILE NAME : N:\PDS\C3D\53100208\SHEETSPLAN\024501-PM.DWG
LAYOUT NAME - 05

PLOT DATE : 10/31/2023 2:58 PM PLOT BY : SHAW, ADAM M PLOT NAME : PLOT SCALE : 1 IN:40 FT

2

24x42" NON-CONDUCTIVE PULLBOX

2" NON-METALLIC CONDUIT

*GRAYSHADE REPRESENTS EXISTING

SEE BLOW-UP A

SEE BLOW-UP B

EXPB16

CB6A

CB3A

EXPB3

CONTRACTOR SHALL:
- REMOVE EXPB19
- INSTALL PB19A
- CONNECT EXPB18 TO PB19A WITH 2" CONDUIT
- CONNECT EXISTING 1" LOOP CONDUIT FROM LOOP 61 TO PB19A

PB19A

EXPB19

EXPB18

INSTALL CONDUIT INTO EXISTING ITEM

EXISTING LOOP 61

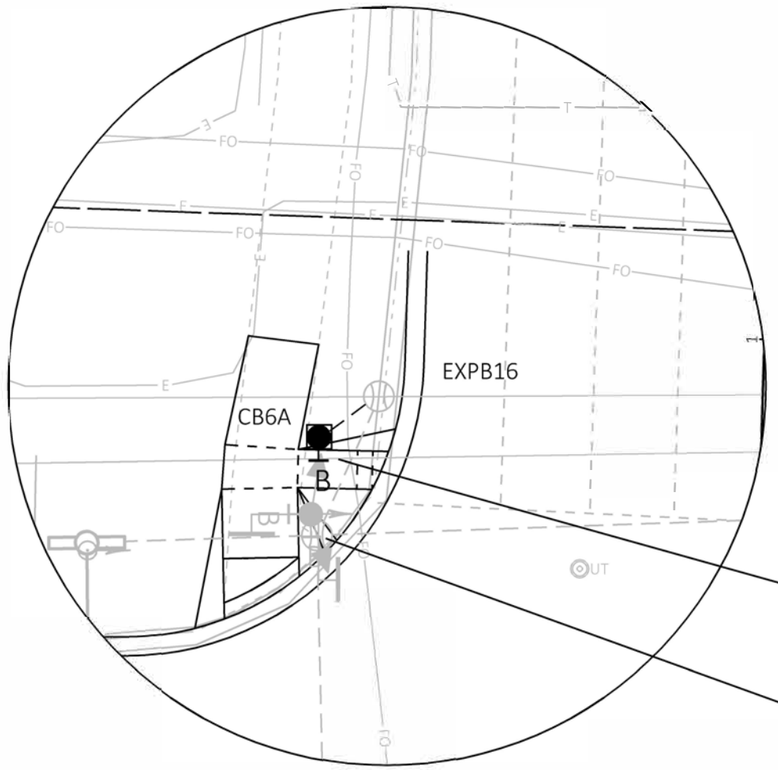
391 392 393 394 395 396

PROJECT NO: 5310-02-78	HWY: USH 14	COUNTY: DANE	TRAFFIC SIGNAL ADJUSTMENTS - USH 14 & DEMING WAY	SHEET 18	E

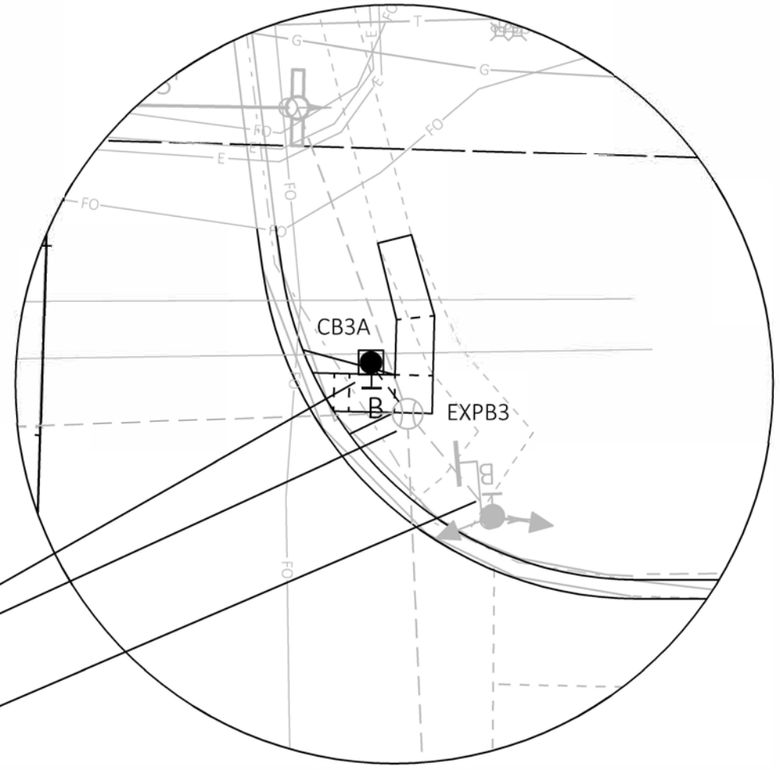
FILE NAME : C:\BOXDRV\BOX\D~SD-PROJECTS-PUBLIC\SWR-PROJECTS-STATE\DESIGN\53100208\TRAFFIC\ELECTRICAL\SIGNALS\CIVIL3D\53100208-SIGNALPLAN.DWG
LAYOUT NAME - 024201-sp
PLOT DATE : 10/23/2023 1:44 PM
PLOT BY : HEITZ, GRAHAM A
PLOT NAME :
PLOT SCALE : 1 IN:40 FT
WISDOT/CADDS SHEET 42

2

BLOW-UP A



BLOW-UP B



LEGEND

- SERVICE POLE
- CONTROL CABINET
- NONMETALLIC CONDUIT 2", UNLESS OTHERWISE NOTED
- LOOP DETECTOR CONDUIT 1" NONMETALLIC
- SIGNAL HEAD, TRAFFIC SIGNAL STANDARD, PEDESTAL BASE
- (XX) MONOTUBE BASE, POLE, 35'-55' ARM
- PEDESTRIAN HEAD WITH PUSH BUTTON
- PUSH BUTTON
- PULL BOX, 24" X 42"
- XX SIGNAL HEAD NUMBER
- R RED CIRCULAR INDICATOR
- Y YELLOW CIRCULAR INDICATOR
- G GREEN CIRCULAR INDICATOR
- RA RED ARROW
- YA YELLOW ARROW
- GA GREEN ARROW
- PEDESTRIAN COUNTDOWN TIMER
- CONCRETE BASES, TYPE 1 (MODIFIED)

NOTES:

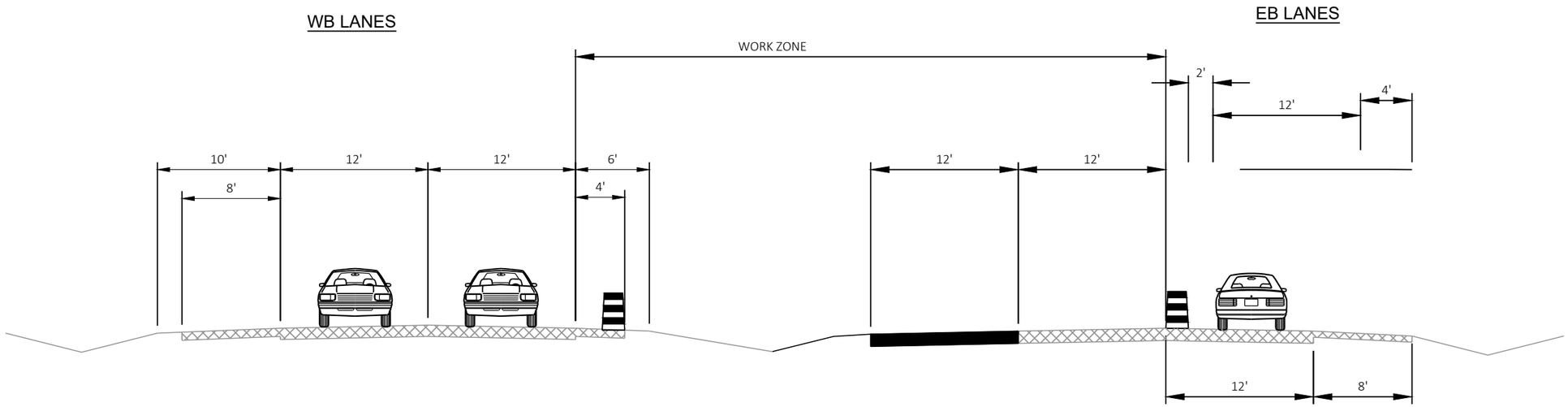
- ALL LENSES ARE 12-INCH
- GRAYSHADE REPRESENTS EXISTING

PLACE CONCRETE BASE SO THAT PEDESTRIAN PUSH BUTTON IS NO MORE THAN 10 FEET FROM THE EDGE OF THE CURB

ADJUST EXISTING PULL BOX

REMOVE EXISTING BUTTON FOR PEDESTRIAN CROSSING OF THE NORTH APPROACH (PHASE 2), EXISTING BUTTON FOR CROSSING OF THE WEST APPROACH IN NW QUADRANT (PHASE 4) TO REMAIN

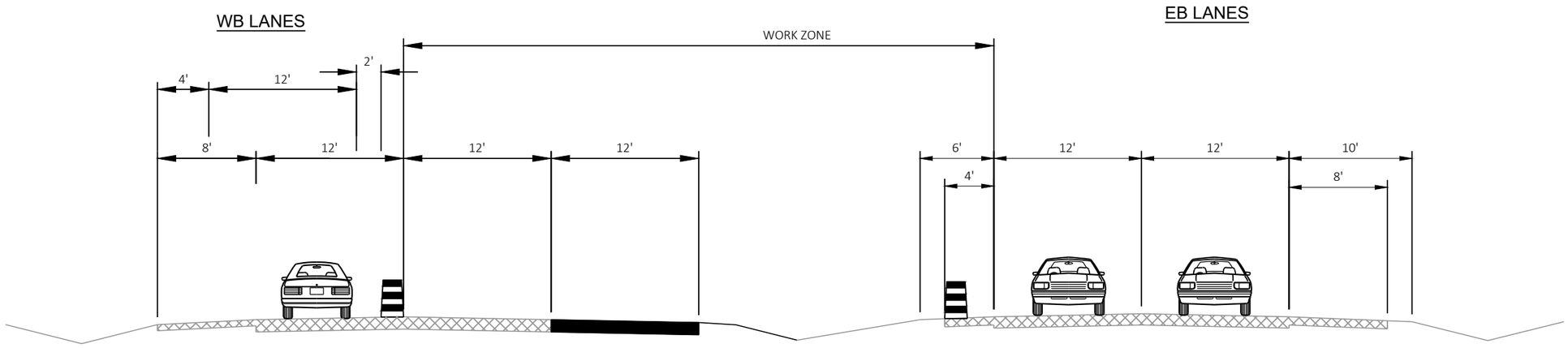
2



USH 14 EASTBOUND STAGING TYPICAL

PROJECT NO: 5310-02-78	HWY: USH 14	COUNTY: DANE	STAGING TYPICAL - USH 14 EASTBOUND	SHEET 20	E

2

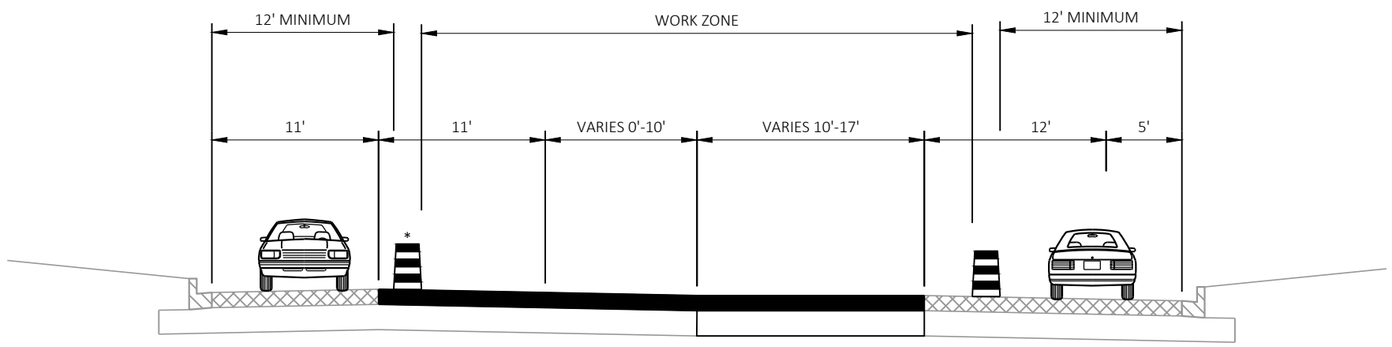


USH 14 WESTBOUND STAGING TYPICAL

PROJECT NO: 5310-02-78	HWY: USH 14	COUNTY: DANE	STAGING TYPICAL - USH 14 WESTBOUND	SHEET 21	E

FILE NAME : \\FIWMAD7P3158\N3PUBLIC\PDS\C3D\53100208\SHEETSPLAN\026001-S1.DWG
LAYOUT NAME - 02-10ft
PLOT DATE : 10/31/2023 8:03 AM
PLOT BY : SHAW, ADAM M
PLOT NAME :
PLOT SCALE : 1 IN:10 FT
WISDOT/CADDS SHEET 42

2



DEMING WAY STAGING TYPICAL

* NORMAL DRUM AND LANE LOCATION WHEN CONSTRUCTION OPERATION IS NOT PRESENT AT THIS LOCATION. WHEN CONSTRUCTION OPERATION OCCURS IN THIS AREA, THE DRUM SHALL BE MOVED TO ACCOMMODATE OPERATION AND RETURNED TO NORMAL POSITION IMMEDIATELY AFTER THE THE OPERATION HAS PASSED. TRAFFIC EXPECTED TO SHIFT ONTO GUTTER TO BYPASS CONSTRUCTION OPERATION.

PROJECT NO: 5310-02-78	HWY: USH 14	COUNTY: DANE	STAGING TYPICAL - DEMING WAY	SHEET 22	E

FILE NAME : \\FIWMAD7P3158\N3PUBLIC\PDS\C3D\53100208\SHEETSPLAN\026001-S1.DWG
LAYOUT NAME - 03-10ft
PLOT DATE : 10/31/2023 8:03 AM PLOT BY : SHAW, ADAM M PLOT NAME : PLOT SCALE : 1 IN:10 FT
WISDOT/CADDS SHEET 42

2

TEMPORARY PEDESTRIAN ACCESS IS
SHOWN ON OTHER PLAN SHEETS

LEGEND

- TYPE III BARRICADE
- TYPE III BARRICADE WTH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
- FLASHING ARROW BOARD
- SIGN ON PERMANENT SUPPORT
- WORK AREA
- DIRECTION OF TRAFFIC
- MB — PORTABLE CHANGEABLE MESSAGE BOARD
- SIGN ON TEMPORARY SUPPORT
- (A) — TYPE A WARNING LIGHT (FLASHING)
- (C) — TYPE C WARNING LIGHT (STEADY BURN)

N

END ROAD WORK
G20-2A
48"X24"

100'

5 DRUMS SPACED AT 10' INTERVALS

ARROW BOARD AT END OF TAPER

540' TAPER

50'

220' BUFFER

390

100'

2'

12'

2'

R11-2L
48" X 30"
LANE CLOSED
TYPE III BARRICADE

R11-2L
48" X 30"
LANE CLOSED
TYPE III BARRICADE

ROAD WORK AHEAD W20-1A

LEFT LANE CLOSED AHEAD W20-5A

W04-2L

500' 500' 1040'

PLACE ADVANCE WARNING SIGNS ON BOTH SIDES OF ROADWAY
AND ON USH 12 EB EXIT RAMP

POST

2

R11-2L
48" X 30"
LANE CLOSED
TYPE III BARRICADE

N

100'

100' TAPER

395

2'

12'

2'

POST

USE SDD:
TRAFFIC CONTROL, SINGLE LANE CLOSURE, NON-FREEWAY/EXPRESSWAY
TRAFFIC CONTROL, SHOULDER CLOSURE ON DIVIDED ROADWAY, SPEEDS GREATER THAN 40 MPH

PROJECT NO: 5310-02-78	HWY: USH 14	COUNTY: DANE	TRAFFIC CONTROL - USH 14 EASTBOUND LEFT TURN LANE EXTENSION	SHEET 23	E

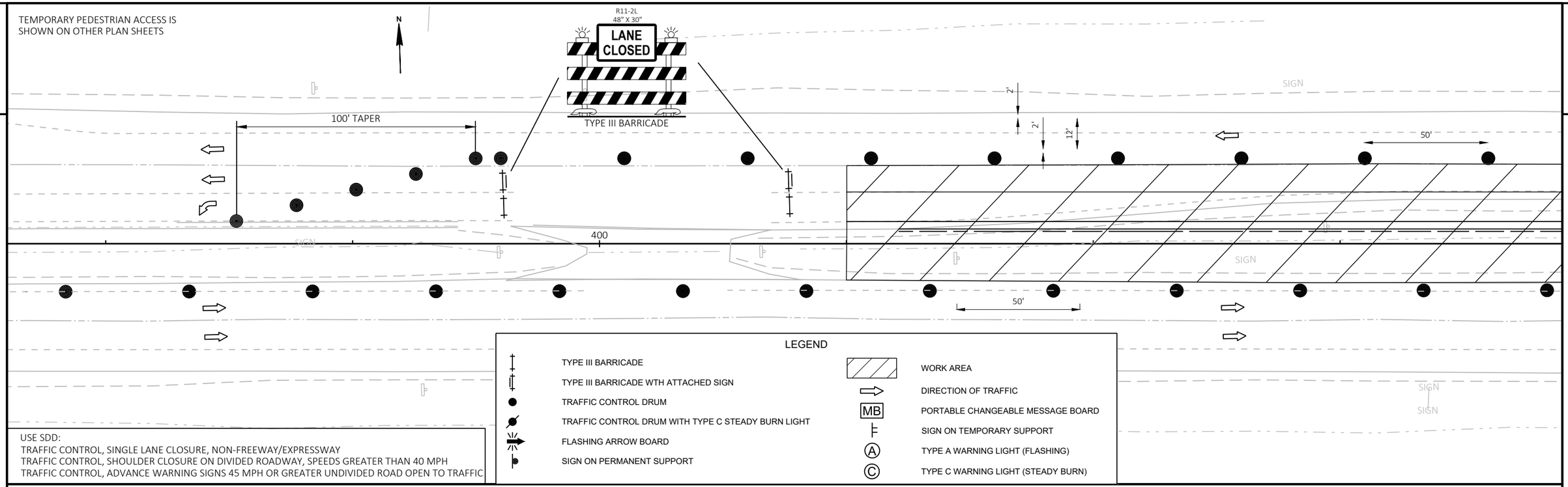
FILE NAME : N:\PDS\C3D\53100208\SHEETSPLAN\025101-TC.DWG
LAYOUT NAME - 05

PLOT DATE : 12/11/2023 11:23 AM PLOT BY : KLUEVER, LUCAS W PLOT NAME : PLOT SCALE : 1 IN:40 FT

WISDOT/CADDS SHEET 44

2

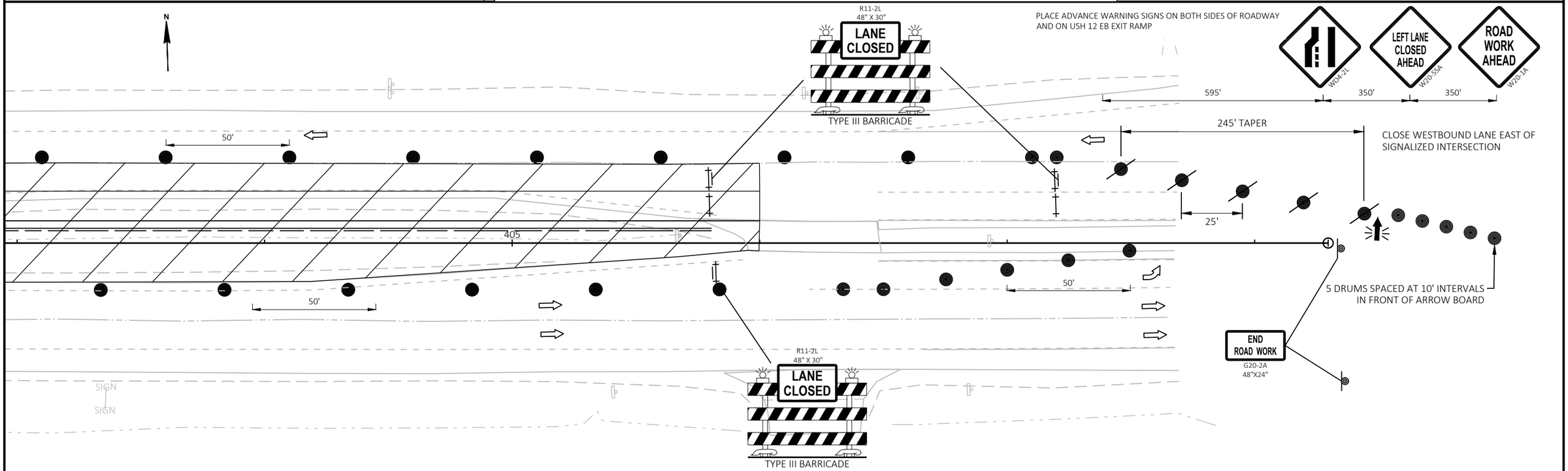
TEMPORARY PEDESTRIAN ACCESS IS
SHOWN ON OTHER PLAN SHEETS



LEGEND

- TYPE III BARRICADE
- TYPE III BARRICADE WTH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
- FLASHING ARROW BOARD
- SIGN ON PERMANENT SUPPORT
- WORK AREA
- DIRECTION OF TRAFFIC
- MB — PORTABLE CHANGEABLE MESSAGE BOARD
- SIGN ON TEMPORARY SUPPORT
- (A) — TYPE A WARNING LIGHT (FLASHING)
- (C) — TYPE C WARNING LIGHT (STEADY BURN)

USE SDD:
TRAFFIC CONTROL, SINGLE LANE CLOSURE, NON-FREEWAY/EXPRESSWAY
TRAFFIC CONTROL, SHOULDER CLOSURE ON DIVIDED ROADWAY, SPEEDS GREATER THAN 40 MPH
TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 MPH OR GREATER UNDIVIDED ROAD OPEN TO TRAFFIC



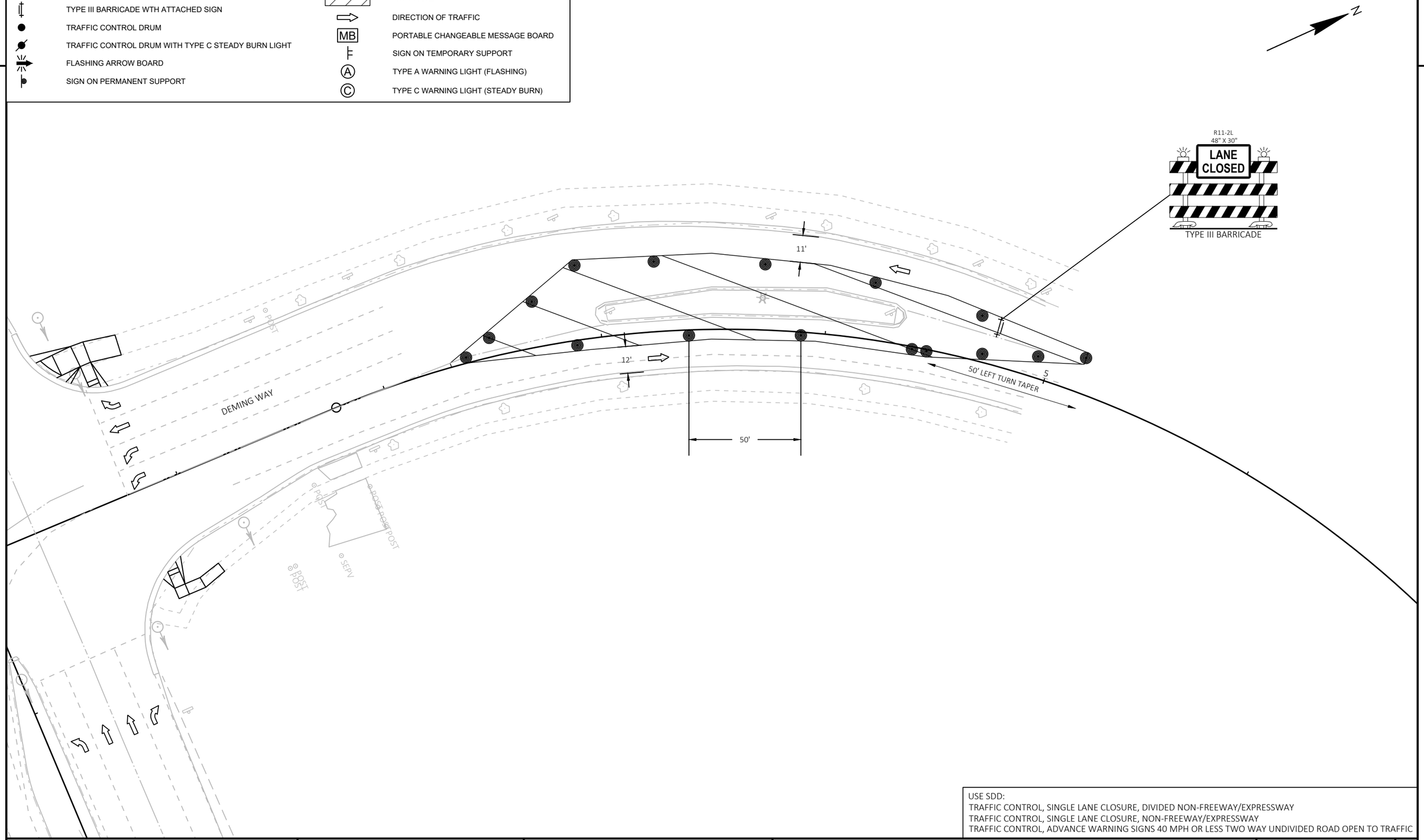
PROJECT NO: 5310-02-78	HWY: USH 14	COUNTY: DANE	TRAFFIC CONTROL - USH 14 WESTBOUND LEFT TURN LANE EXTENSION	SHEET 24	E

2

LEGEND

- TYPE III BARRICADE
- TYPE III BARRICADE WTH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
- FLASHING ARROW BOARD
- SIGN ON PERMANENT SUPPORT
- WORK AREA
- DIRECTION OF TRAFFIC
- MB — PORTABLE CHANGEABLE MESSAGE BOARD
- SIGN ON TEMPORARY SUPPORT
- A — TYPE A WARNING LIGHT (FLASHING)
- C — TYPE C WARNING LIGHT (STEADY BURN)

TEMPORARY PEDESTRIAN ACCESS IS
SHOWN ON OTHER PLAN SHEETS



USE SDD:
TRAFFIC CONTROL, SINGLE LANE CLOSURE, DIVIDED NON-FREEWAY/EXPRESSWAY
TRAFFIC CONTROL, SINGLE LANE CLOSURE, NON-FREEWAY/EXPRESSWAY
TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 MPH OR LESS TWO WAY UNDIVIDED ROAD OPEN TO TRAFFIC

PROJECT NO: 5310-02-78	HWY: USH 14	COUNTY: DANE	TRAFFIC CONTROL - DEMING WAY LEFT TURN LANE EXTENSION	SHEET 25	E

2

LEGEND

TYPE III BARRICADE WTH ATTACHED SIGN

SIGN ON PERMANENT SUPPORT

SIGN ON TEMPORARY SUPPORT

N

DETOUR
M4-9BL
30"X24"

SIDEWALK CLOSED AHEAD
R9-9A
30"X18"

DETOUR
M4-9BR
30"X24"

DETOUR
M4-9BL
30"X24"

SIDEWALK CLOSED AHEAD
R9-9A
30"X18"

SIDEWALK CLOSED
R9-9
24"X12"

MURPHY DRIVE

DEMING WAY

PC: 1+77.04

PC: 391+22.33

PT: 395+29.69

BP: 0+00.00

390

395

400

TEMPORARY PEDESTRIAN BARRICADE

USH 14 WB

USH 14 EB

2

PROJECT NO: 5310-02-78	HWY: USH 14	COUNTY: DANE	TEMPORARY PEDESTRIAN ACCESS AND DETOUR	SHEET 26	E

FILE NAME : N:\PDS\C3D\53100208\SHEETSPLAN\027001-DT.DWG
LAYOUT NAME - 01

PLOT DATE : 12/11/2023 11:28 AM PLOT BY : KLUEVER, LUCAS W PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 42

2

LEGEND

TYPE III BARRICADE WTH ATTACHED SIGN

SIGN ON PERMANENT SUPPORT

SIGN ON TEMPORARY SUPPORT

N

MURPHY DRIVE

DEMING WAY

SIDEWALK CLOSED AHEAD
R9-9A
30"X18"

TEMPORARY PEDESTRIAN BARRICADE

PC: 1+77.04

SIDEWALK CLOSED
R9-9
24"X12"

PC: 391+22.33

PT: 395+29.69

BP: 0+00.00

390

395

400

USH 14 WB

USH 14 EB

TEMPORARY PEDESTRIAN BARRICADE

SIDEWALK CLOSED AHEAD
R9-9A
30"X18"

SIDEWALK CLOSED
R9-9
24"X12"

PROJECT NO: 5310-02-78	HWY: USH 14	COUNTY: DANE	TEMPORARY PEDESTRIAN ACCESS AND DETOUR	SHEET 27	E

FILE NAME : N:\PDS\C3D\53100208\SHEETSPLAN\027002-DT.DWG
LAYOUT NAME - 01

PLOT DATE : 12/11/2023 11:29 AM PLOT BY : KLUEVER, LUCAS W PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 42

Estimate Of Quantities

5310-02-78

Line	Item	Item Description	Unit	Total	Qty
0002	204.0100	Removing Concrete Pavement	SY	115.000	115.000
0004	204.0110	Removing Asphaltic Surface	SY	234.000	234.000
0006	204.0150	Removing Curb & Gutter	LF	445.000	445.000
0008	204.0155	Removing Concrete Sidewalk	SY	110.000	110.000
0010	205.0100	Excavation Common	CY	1,207.000	1,207.000
0012	213.0100	Finishing Roadway (project) 01. 5310-02-78	EACH	1.000	1.000
0014	305.0110	Base Aggregate Dense 3/4-Inch	TON	60.000	60.000
0016	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	942.000	942.000
0018	305.0130	Base Aggregate Dense 3-Inch	TON	870.000	870.000
0020	455.0605	Tack Coat	GAL	172.000	172.000
0022	460.2000	Incentive Density HMA Pavement	DOL	370.000	370.000
0024	460.6424	HMA Pavement 4 MT 58-28 H	TON	569.000	569.000
0026	465.0105	Asphaltic Surface	TON	12.000	12.000
0028	601.0411	Concrete Curb & Gutter 30-Inch Type D	LF	209.000	209.000
0030	601.0600	Concrete Curb Pedestrian	LF	20.000	20.000
0032	602.0410	Concrete Sidewalk 5-Inch	SF	313.000	313.000
0034	602.0505	Curb Ramp Detectable Warning Field Yellow	SF	60.000	60.000
0036	602.0605	Curb Ramp Detectable Warning Field Radial Yellow	SF	48.000	48.000
0038	618.0100	Maintenance and Repair of Haul Roads (project) 01. 5310-02-78	EACH	1.000	1.000
0040	619.1000	Mobilization	EACH	1.000	1.000
0042	624.0100	Water	MGAL	12.000	12.000
0044	625.0500	Salvaged Topsoil	SY	262.000	262.000
0046	628.1504	Silt Fence	LF	60.000	60.000
0048	628.1520	Silt Fence Maintenance	LF	60.000	60.000
0050	628.1905	Mobilizations Erosion Control	EACH	2.000	2.000
0052	628.1910	Mobilizations Emergency Erosion Control	EACH	2.000	2.000
0054	628.2008	Erosion Mat Urban Class I Type B	SY	262.000	262.000
0056	628.7005	Inlet Protection Type A	EACH	4.000	4.000
0058	628.7015	Inlet Protection Type C	EACH	2.000	2.000
0060	629.0210	Fertilizer Type B	CWT	0.700	0.700
0062	630.0130	Seeding Mixture No. 30	LB	20.000	20.000
0064	630.0500	Seed Water	MGAL	1.000	1.000
0066	634.0616	Posts Wood 4x6-Inch X 16-FT	EACH	2.000	2.000
0068	638.2102	Moving Signs Type II	EACH	2.000	2.000
0070	638.2602	Removing Signs Type II	EACH	1.000	1.000
0072	638.3000	Removing Small Sign Supports	EACH	1.000	1.000
0074	642.5001	Field Office Type B	EACH	1.000	1.000
0076	643.0300	Traffic Control Drums	DAY	1,861.000	1,861.000
0078	643.0420	Traffic Control Barricades Type III	DAY	170.000	170.000
0080	643.0705	Traffic Control Warning Lights Type A	DAY	300.000	300.000
0082	643.0715	Traffic Control Warning Lights Type C	DAY	250.000	250.000
0084	643.0800	Traffic Control Arrow Boards	DAY	24.000	24.000
0086	643.0900	Traffic Control Signs	DAY	1,523.000	1,523.000
0088	643.1050	Traffic Control Signs PCMS	DAY	14.000	14.000
0090	643.5000	Traffic Control	EACH	1.000	1.000
0092	644.1430	Temporary Pedestrian Surface Plate	SF	280.000	280.000
0094	644.1440	Temporary Pedestrian Surface Matting	SF	310.000	310.000
0096	644.1601	Temporary Pedestrian Curb Ramp	DAY	14.000	14.000
0098	644.1605	Temporary Pedestrian Detectable Warning Field	SF	50.000	50.000
0100	644.1810	Temporary Pedestrian Barricade	LF	77.000	77.000

5310-02-78

Line	Item	Item Description	Unit	Total	Qty
0102	646.1005	Marking Line Paint 4-Inch	LF	710.000	710.000
0104	646.2020	Marking Line Epoxy 6-Inch	LF	840.000	840.000
0106	646.3005	Marking Line Paint 8-Inch	LF	326.000	326.000
0108	646.4020	Marking Line Epoxy 10-Inch	LF	735.000	735.000
0110	646.6105	Marking Stop Line Paint 18-Inch	LF	46.000	46.000
0112	646.7405	Marking Crosswalk Paint Transverse Line 6-Inch	LF	164.000	164.000
0114	646.7420	Marking Crosswalk Epoxy Transverse Line 6-Inch	LF	216.000	216.000
0116	646.9000	Marking Removal Line 4-Inch	LF	305.000	305.000
0118	646.9002	Marking Removal Line 6-Inch	LF	260.000	260.000
0120	650.4500	Construction Staking Subgrade	LF	1,056.000	1,056.000
0122	650.5000	Construction Staking Base	LF	1,056.000	1,056.000
0124	650.5500	Construction Staking Curb Gutter and Curb & Gutter	LF	309.000	309.000
0126	650.8501	Construction Staking Electrical Installations (project) 01. 5310-02-78	EACH	1.000	1.000
0128	650.9000	Construction Staking Curb Ramps	EACH	6.000	6.000
0130	650.9500	Construction Staking Sidewalk (project) 01. 5310-02-78	EACH	1.000	1.000
0132	650.9911	Construction Staking Supplemental Control (project) 01. 5310-02-78	EACH	1.000	1.000
0134	650.9920	Construction Staking Slope Stakes	LF	915.000	915.000
0136	652.0225	Conduit Rigid Nonmetallic Schedule 40 2-Inch	LF	150.000	150.000
0138	652.0800	Conduit Loop Detector	LF	30.000	30.000
0140	653.0164	Pull Boxes Non-Conductive 24x42-Inch	EACH	1.000	1.000
0142	653.0900	Adjusting Pull Boxes	EACH	1.000	1.000
0144	653.0905	Removing Pull Boxes	EACH	1.000	1.000
0146	655.0700	Loop Detector Lead In Cable	LF	930.000	930.000
0148	655.0800	Loop Detector Wire	LF	360.000	360.000
0150	657.0100	Pedestal Bases	EACH	2.000	2.000
0152	657.0405	Traffic Signal Standards Aluminum 3.5-FT	EACH	2.000	2.000
0154	658.0500	Pedestrian Push Buttons	EACH	2.000	2.000
0156	690.0150	Sawing Asphalt	LF	1,487.000	1,487.000
0158	690.0250	Sawing Concrete	LF	186.000	186.000
0160	ASP.1T0A	On-the-Job Training Apprentice at $5.00/HR	HRS	200.000	200.000
0162	ASP.1T0G	On-the-Job Training Graduate at $5.00/HR	HRS	175.000	175.000
0164	SPV.0060	Special 01. Concrete Bases, Type 1 (Modified)	EACH	2.000	2.000
0166	SPV.0090	Special 01. Concrete Curb & Gutter 30-Inch Type D HES	LF	100.000	100.000
0168	SPV.0165	Special 01. Concrete Sidewalk 5-Inch HES	SF	820.000	820.000

3

204 - REMOVALS

CATEGORY	STATION	TO	STATION	LOCATION	204.0100 REMOVING CONCRETE PAVEMENT SY	204.0110 REMOVING ASPHALTIC SURFACE SY	204.0150 REMOVING CURB & GUTTER LF	204.0155 REMOVING CONCRETE SIDEWALK SY
0010	STA 2+94 D	-	STA 4+00 D	LT	-	200	295	-
0010	STA 391+55	-	STA 392+50	RT	115	-	-	-
0010	STA 394+42	-	STA 394+83	LT	-	14	60	37
0010	STA 394+46	-	STA 394+81	RT	-	9	40	30
0010	STA 394+56	-	STA 394+69	LT & RT	-	-	-	15
0010	STA 395+56	-	STA 395+89	LT	-	11	50	28
				TOTAL 0010	115	234	445	110

205 - EARTHWORK

CATEGORY	STATION	TO	STATION	LOCATION	205.0100 EXCAVATION COMMON CY	UNUSABLE PAVEMENT MATERIAL CY	AVAILABLE MATERIAL CY	UNEXPANDED FILL CY	EXPANDED FILL (FACTOR 1.25) CY	MASS ORDINATE/WASTE CY *
0010	STA 388+35	-	STA 392+50	EB LEFT TURN LANE	429	61	368	4	5	424
0010	STA 401+00	-	STA 406+00	WB LEFT TURN LANE	582	60	522	0	0	582
0010	STA 2+94 D	-	STA 4+35 D	DEMING WAY	111	14	97	0	0	111
0010		-		SIDEWALK AREAS	40	40	0	0	0	40
0010		-		EBS	45	0	45	0	0	45
0010		-								
				TOTAL 0010	1,207	175	1032	4	5	1202

* MASS ORDINATE/WASTE = EXCAVATION COMMON - EXPANDED FILL

305 - BASE

CATEGORY	STATION	TO	STATION	LOCATION	305.0110 BASE AGGREGATE DENSE 3/4-INCH TON	305.0120 BASE AGGREGATE DENSE 1 1/4-INCH TON	305.0130 BASE AGGREGATE DENSE 3-INCH TON
0010	STA 388+35	-	STA 392+50	RT, EB LEFT TURN LANE	20	312	346
0010	STA 401+00	-	STA 406+00	RT, WB LEFT TURN LANE	40	456	464
0010	STA 2+94 D	-	STA 4+35 D	LT, DEMING WAY	-	118	-
0010		-		SIDEWALK	-	28	-
0010		-		UNDISTRIBUTED, EBS	-	28	60
				TOTAL 0010	60	942	870

460 - ASPHALT

CATEGORY	STATION	TO	STATION	LOCATION	455.0605 TACK COAT GAL	460.6424 HMA PAVEMENT 4 MT 58-28 H TON	465.0105 ASPHALTIC SURFACE TON
0010	STA 388+35	-	STA 392+50	EASTBOUND	50	169	-
0010	STA 401+00	-	STA 406+00	WESTBOUND	78	262	-
0010	STA 2+94 D	-	STA 4+35 D	DEMING WAY MEDIAN	41	138	-
0010		-		CURB REMOVAL AREAS	3	-	12
				TOTAL 0010	172	569	12

601 - CONCRETE ITEMS

CATEGORY	STATION	TO	STATION	LOCATION	601.0411 CONCRETE CURB & GUTTER 30-INCH TYPE D LF	601.0600 CONCRETE CURB PEDESTRIAN LF	602.0410 CONCRETE SIDEWALK 5-INCH SF	602.0505 CURB RAMP DETECTABLE WARNING FIELD YELLOW SF	602.0605 CURB RAMP DETECTABLE WARNING FIELD RADIAL YELLOW SF	SPV.0090.01 SPECIAL (01. CONCRETE CURB & GUTTER 30-INCH TYPE D HES) LF	SPV.0165.01 SPECIAL (01. CONCRETE SIDEWALK 5-INCH HES) SF
0010	STA 394+42	-	STA 394+83	LT, NW QUADRANT	-	-	118	10	24	60	395
0010	STA 394+46	-	STA 394+81	RT, SW QUADRANT	-	-	-	-	24	40	295
0010	STA 394+58	-	STA 394+69	LT & RT, MEDIAN	-	20	-	40	-	-	130
0010	STA 395+56	-	STA 395+89	LT, NE QUADRANT	50	-	195	10	-	-	-
0010	STA 390+96	-	STA 392+50	RT, EB	159	-	-	-	-	-	-
				TOTAL 0010	209	20	313	60	48	100	820

3

625 - FINISHING ITEMS

CATEGORY	STATION	TO	STATION	LOCATION	625.0500 SALVAGED TOPSOIL SY	629.0210 FERTILIZER TYPE B CWT	630.0130 SEEDING MIXTURE NO. 30 LB	630.0500 SEED WATER MGAL
0010	STA 388+35	-	STA 392+50	EB USH 14	84	0.2	6	-
0010	STA 401+00	-	STA 406+00	WB USH 14	-	0.2	5	-
0010				SIDEWALK	148	0.2	5	-
0010				PROJECT	-	-	-	-
0010				UNDISTRIBUTED	30	0.1	4	1
0010				TOTAL 0010	262	0.7	20	1

628 - EROSION CONTROL

CATEGORY	STATION	TO	STATION	LOCATION	628.1504 SILT FENCE LF	628.1520 SILT FENCE MAINTENANCE LF	628.2008 EROSION MAT URBAN CLASS I TYPE B SY	628.7005 INLET PROTECTION TYPE A EACH	628.7015 INLET PROTECTION TYPE C EACH
0010	STA 388+35	-	STA 392+50	EB USH 14	-	-	84	-	-
0010	STA 401+00	-	STA 406+00	WB USH 14	-	-	-	-	-
0010				SIDEWALK	-	-	148	-	-
0010				PROJECT	40	40	-	4	2
0010				UNDISTRIBUTED	20	20	30	-	-
0010				TOTAL 0010	60	60	262	4	2

628 - MOB EROSION CONTROL

CATEGORY	STATION	TO	STATION	LOCATION	628.1905 MOBILIZATIONS EROSION CONTROL EACH	628.1910 MOBILIZATIONS EMERGENCY EROSION CONTROL EACH
0010		-		PROJECT	2	2
				TOTAL 0010	2	2

624 - WATER

CATEGORY	STATION	TO	STATION	LOCATION	624.0100 WATER MGAL
0010		-		PROJECT	12
				TOTAL 0010	12

638 - SIGNS

CATEGORY	MOVE FROM	TO	STATION	LOCATION	634.0616 POSTS WOOD 4X6-INCH X 16-FT EACH	638.2102 MOVING SIGNS TYPE II EACH	638.2602 REMOVING SIGNS TYPE II EACH	638.3000 REMOVING SMALL SIGN SUPPORTS EACH	REMARKS
0010	390+00	-	389+90	MEDIAN	1	1	-	-	KEEP RIGHT
0010	405+65	-	405+15	MEDIAN	1	1	-	-	KEEP RIGHT
0010		-	402+95	MEDIAN	-	-	1	1	BEGIN LEFT TURN LANE
				TOTAL 0010	2	2	1	1	

643 - TRAFFIC CONTROL

CATEGORY	LOCATION	DAYS IN SERVICE	643.0300 TRAFFIC CONTROL DRUMS NO.	643.0300 TRAFFIC CONTROL DRUMS DAY	643.0420 TRAFFIC CONTROL BARRICADES TYPE III NO.	643.0420 TRAFFIC CONTROL BARRICADES TYPE III DAY	643.0705 TRAFFIC CONTROL WARNING LIGHTS TYPE A NO.	643.0705 TRAFFIC CONTROL WARNING LIGHTS TYPE A DAY	643.0715 TRAFFIC CONTROL WARNING LIGHTS TYPE C NO.	643.0715 TRAFFIC CONTROL WARNING LIGHTS TYPE C DAY	643.0800 TRAFFIC CONTROL ARROW BOARDS NO.	643.0800 TRAFFIC CONTROL ARROW BOARDS DAY	643.0900 TRAFFIC CONTROL SIGNS NO.	643.0900 TRAFFIC CONTROL SIGNS DAY	643.1050 TRAFFIC CONTROL SIGNS PCMS DAY	643.5000 TRAFFIC CONTROL EACH
0010	EB TURN LANE	10	49	490	3	30	6	60	11	110	1	10	16	160	-	-
0010	WB TURN LANE	9	68	612	9	81	18	162	10	90	1	9	16	144	-	-
0010	DEMING WAY	9	16	144	1	9	2	18	-	-	-	-	11	99	-	-
0010	CURB RAMPS	16	15	240	1	16	-	-	-	-	-	-	10	160	-	-
0010	UNDISTRIBUTED		-	375	-	34	-	60	-	50	-	5		300	-	-
0010	PROJECT	55	-	-	-	-	-	-	-	-	-	-	12	660	14	1
	TOTAL 0010			1,861		170		300		250		24		1,523	14	1

644-TEMPORARY PEDESTRIAN ITEMS

CATEGORY	STATION	TO	STATION	LOCATION	644.1430 TEMPORARY PEDESTRIAN SURFACE PLATE SF	644.1440 TEMPORARY PEDESTRIAN SURFACE MATTING SF	644.1601 TEMPORARY PEDESTRIAN CURB RAMP DAY	644.1605 TEMPORARY PEDESTRIAN DETECTABLE WARNING FIELD SF	644.1810 TEMPORARY PEDESTRIAN BARRICADE LF
0010				NW QUADRANT	180	-	3	10	21
0010				NE QUADRANT	-	-	-	-	15
0010				MEDIAN	-	60	3	20	-
0010				SW QUADRANT	-	150	3	10	21
0010				UNDISTRIBUTED	100	100	5	10	20
0010				TOTAL 0010	280	310	14	50	77

646 - PAVEMENT MARKING

CATEGORY	STATION	TO	STATION	LOCATION	MARKING LINE PAINT 4-INCH LF (WHITE)	646.1005 MARKING LINE PAINT 4-INCH LF (YELLOW)	646.2020 MARKING LINE EPOXY 6-INCH LF	646.3005 MARKING LINE PAINT 8-INCH LF	646.4020 MARKING LINE EPOXY 10-INCH LF	646.6105 MARKING STOP LINE PAINT 18-INCH LF	646.7405 MARKING CROSSWALK PAINT TRANSVERSE LINE 6-INCH LF	646.7420 MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH LF	646.9000 MARKING REMOVAL LINE 4-INCH LF	646.9002 MARKING REMOVAL LINE 6-INCH LF
0010		-		DEMING WAY	130	580	-	326	-	46	164	-	305	260
0010		-		EB USH 14 LEFT TURN LANE	-	-	360	-	255	-	-	-	-	-
0010		-		WB USH 14 LEFT TURN LANE	-	-	480	-	480	-	-	-	-	-
0010		-		USH 14 CROSSWALK	-	-	-	-	-	-	-	216	-	-
0010		-		SUBTOTAL	130	580	840	326	735	46	164	216	305	260
0010		-		TOTAL 0010		710	840	326	735	46	164	216	305	260

650 - CONSTRUCTION STAKING

CATEGORY	STATION	TO	STATION	LOCATION	650.4500 CONSTRUCTION STAKING SUBGRADE LF	650.5000 CONSTRUCTION STAKING BASE LF	650.5500 CONSTRUCTION STAKING CURB GUTTER AND CURB & GUTTER LF	650.8501.01 CONSTRUCTION STAKING ELECTRICAL INSTALLATIONS (PROJECT) (5310-02-78) EACH	650.9000 CONSTRUCTION STAKING CURB RAMPS EACH	650.9500.01 CONSTRUCTION STAKING SIDEWALK (PROJECT) (5310-02-78) EACH	650.9911.01 CONSTRUCTION STAKING SUPPLEMENTAL CONTROL (PROJECT) (5310-02-78) EACH	650.9920 CONSTRUCTION STAKING SLOPE STAKES LF
0010	STA 388+35	-	STA 392+50	RT, EB LEFT TURN LANE	415	415	159	-	-	-	-	415
0010	STA 401+00	-	STA 406+00	RT, WB LEFT TURN LANE	500	500	-	-	-	-	-	500
0010	STA 2+94 D	-	STA 4+35 D	LT, DEMING WAY	141	141	-	-	-	-	-	-
0010	STA 394+42	-	STA 394+83	LT, NW QUADRANT	-	-	60	-	2	-	-	-
0010	STA 394+46	-	STA 394+81	RT, SW QUADRANT	-	-	40	-	1	-	-	-
0010	STA 394+58	-	STA 394+69	LT & RT, MEDIAN	-	-	-	-	2	-	-	-
0010	STA 395+56	-	STA 395+89	LT, NE QUADRANT	-	-	50	-	1	-	-	-
0010				PROJECT	-	-	-	1		1	1	-
				TOTAL 0010	1,056	1,056	309	1	6	1	1	915

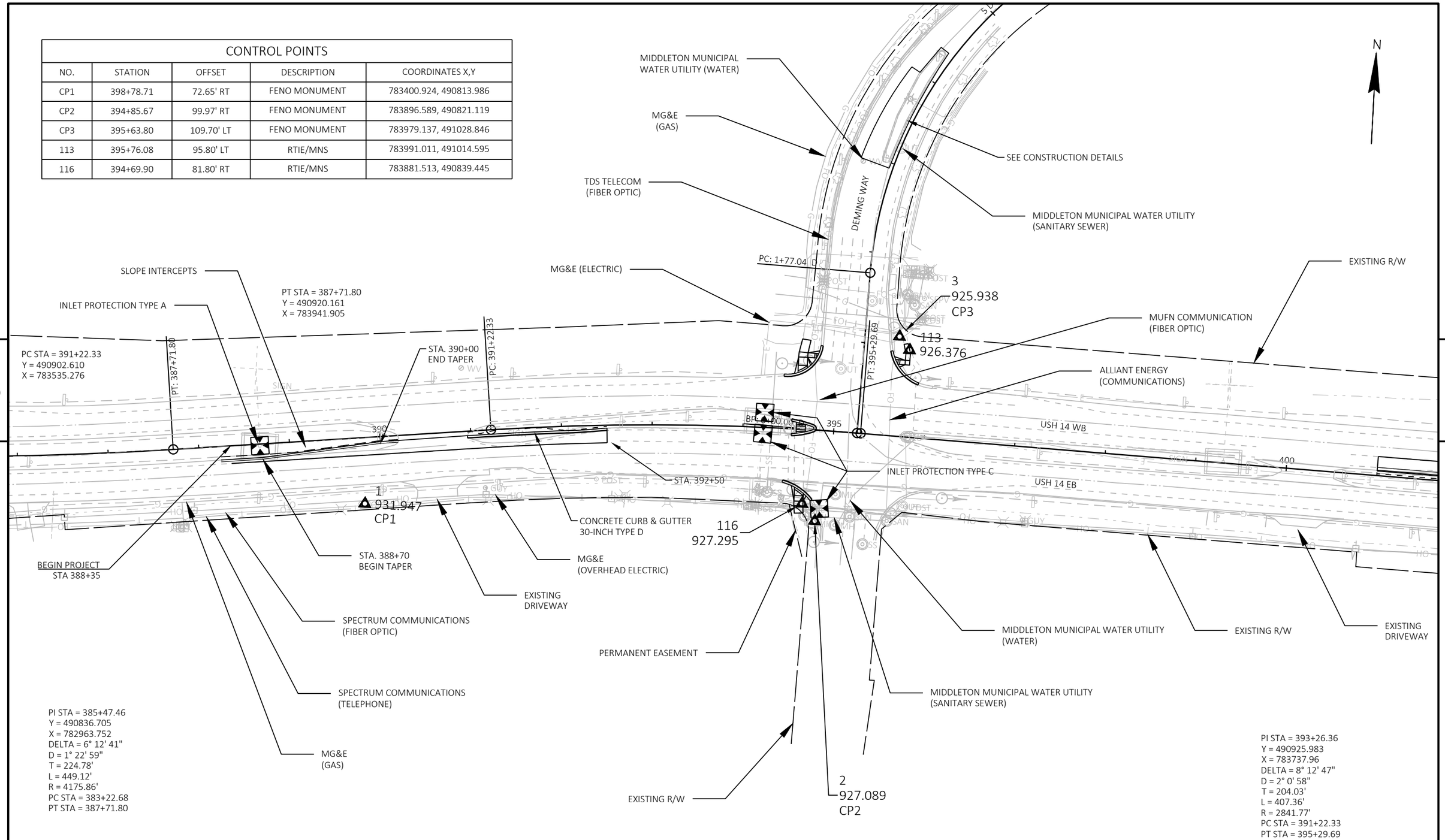
653 - SIGNALS

CATEGORY	FROM		TO	LOCATION	652.0225 CONDUIT RIGID NONMETALLIC SCHEDULE 40 2-INCH LF	652.0800 CONDUIT LOOP DETECTOR LF	653.0164 PULL BOXES NON-CONDUCTIVE 24X42-INCH EACH	653.0900 ADJUSTING PULL BOXES EACH	653.0905 REMOVING PULL BOXES EACH	655.0700 LOOP DETECTOR LEAD IN CABLE LF	655.0800 LOOP DETECTOR WIRE LF	657.0100 PEDESTAL BASES EACH	657.0405 TRAFFIC SIGNAL STANDARDS ALUMINUM 3.5-FT EACH	658.0500 PEDESTRIAN PUSH BUTTONS EACH	SPV.0060.01 SPECIAL (01. CONCRETE BASES, TYPE 1 (MODIFIED)) EACH
0010	PB19A	-		391+25 1' LT	-	-	1	-	-	-	-	-	-	-	-
0010	EXPB3	-		395+73 75' LT	-	-	-	1	-	-	-	-	-	-	-
0010	EXPB19	-		391+25 10' RT		-	-	-	1	-	-	-	-	-	-
	EXPB3	-	CB3A		10	-	-	-	-	-	-	-	-	-	-
0010	EXPB16	-	CB6A		10	-	-	-	-	-	-	-	-	-	-
0010	EXPB18	-	PB19A		130	-	-	-	-	-	-	-	-	-	-
0010	CB3A	-		395+70 80' LT	-	-	-	-	-	-	-	1	1	1	1
0010	CB6A	-		394+70 85' LT	-	-	-	-	-	-	-	1	1	1	1
0010	CAB	-	CB3A	PH 2 PED PUSH BUTTON	-	-	-	-	-	315	-	-	-	-	-
0010	CAB	-	CB6A	PH 2 PED PUSH BUTTON	-	-	-	-	-	165	-	-	-	-	-
0010	CAB	-	PB19A	VEH LOOP NO 61	-	-	-	-	-	450	-	-	-	-	-
0010		-		VEH LOOP NO 61	-	30	-	-			360	-	-	-	-
				TOTAL 0010	150	30	1	1	1	930	360	2	2	2	2

690 - SAWING

CATEGORY	STATION	TO	STATION	LOCATION	690.0150 SAWING ASPHALT LF	690.0250 SAWING CONCRETE LF
0010	STA 388+35	-	STA 392+50	EASTBOUND	325	116
0010	STA 401+00	-	STA 406+00	WESTBOUND	530	-
0010	STA 2+94 D	-	STA 4+35 D	DEMING WAY MEDIAN	460	-
0010				SIDEWALK	10	57
0010				CURB REPLACEMENT	162	13
0010						
				TOTAL 0010	1,487	186

CONTROL POINTS				
NO.	STATION	OFFSET	DESCRIPTION	COORDINATES X,Y
CP1	398+78.71	72.65' RT	FENO MONUMENT	783400.924, 490813.986
CP2	394+85.67	99.97' RT	FENO MONUMENT	783896.589, 490821.119
CP3	395+63.80	109.70' LT	FENO MONUMENT	783979.137, 491028.846
113	395+76.08	95.80' LT	RTIE/MNS	783991.011, 491014.595
116	394+69.90	81.80' RT	RTIE/MNS	783881.513, 490839.445



PROJECT NO: 5310-02-78	HWY: USH 14	COUNTY: DANE	PLAN	SHEET 36	E

FILE NAME : N:\PDS\C3D\53100208\SHEETSPLAN\050201-PN.DWG
LAYOUT NAME - 01

PLOT DATE : 10/31/2023 8:04 AM PLOT BY : SHAW, ADAM M PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44

CONTROL POINTS				
NO.	STATION	OFFSET	DESCRIPTION	COORDINATES X,Y
CP2	394+85.67	99.97' RT	FENO MONUMENT	783896.589, 490821.119
CP3	395+63.80	109.70' LT	FENO MONUMENT	783979.137, 491028.846
CP4	403+95.13	59.98' LT	FENO MONUMENT	784808.704, 490955.416
113	395+76.08	95.80' LT	RTIE/MNS	783991.011, 491014.595
116	394+69.90	81.80' RT	RTIE/MNS	783881.513, 490839.445

N

SEE CONSTRUCTION DETAILS

PC STA = 1+77.04 'D'
Y = 491097.233
X = 783943.219

PI STA = 6+24.34 'D'
Y = 491544.338
X = 783956.398
DELTA = 89° 39' 18"
D = 12° 43' 57"
T = 447.3'
L = 704.15'
R = 450.00'
PC STA = 1+77.04 'D'
PT STA = 8+81.19 'D'

PI STA = 0+00 'D'
Y = 490920.269
X = 783938.003

PI STA = 408+29.69
Y = 490883.066
X = 785241.375

EXISTING R/W

STA. 401+00

END PROJECT
STA. 406+00

3
925.938
CP3

113
926.376

4
919.480
CP4

ALLIANT ENERGY
(COMMUNICATIONS)

PC: 1+77.04 'D'

PT: 395+29.69

EP: 408+29.69

USH 14 WB

USH 14 EB

395

400

405

116
927.295

STA 395+25.78 =
STA 0+00 'D'

EXISTING
DRIVEWAY

EXISTING R/W

EXISTING
DRIVEWAY

INLET PROTECTION TYPE A
EXISTING INLET TO REMAIN

PERMANENT EASEMENT

MUFN COMMUNICATION
(FIBER OPTIC)

SPECTRUM COMMUNICATIONS
(TELEPHONE)

MG&E
(GAS)

MG&E
(OVERHEAD ELECTRIC)

2
927.089
CP2

PROJECT NO: 5310-02-78	HWY: USH 14	COUNTY: DANE	PLAN	SHEET 37	E

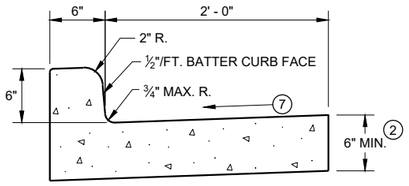
FILE NAME : N:\PDS\C3D\53100208\SHEETSPLAN\050201-PN.DWG
LAYOUT NAME - 02

PLOT DATE : 10/31/2023 8:04 AM PLOT BY : SHAW, ADAM M PLOT NAME : PLOT SCALE : 1 IN:100 FT

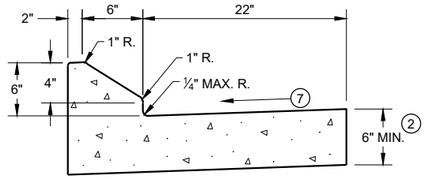
WISDOT/CADDS SHEET 44

Standard Detail Drawing List

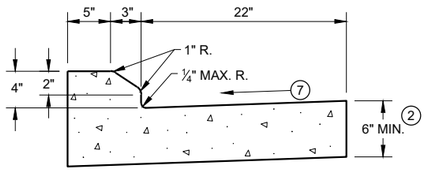
08D01-23A	CONCRETE CURB & GUTTER
08D01-23B	CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS
08D05-21A	CURB RAMPS TYPES 1 AND 1-A
08D05-21B	CURB RAMPS TYPES 2 AND 3
08D05-21C	CURB RAMPS TYPES 4A AND 4A1
08D05-21D	CURB RAMPS TYPE 4B AND 4B1
08D05-21E	CURB RAMPS TYPES 5, 6, 7A, 7B & 8
08D05-21F	CURB RAMPS RADIAL DETECTABLE WARNING FIELD APPLICATIONS
08D05-21G	CURB RAMPS RECTANGULAR AND RADIAL DETECTABLE WARNING PLATES
08E09-06	SILT FENCE
08E10-02	INLET PROTECTION TYPE A, B, C AND D
09B02-10	CONDUIT
09B16-02	PULL BOX NON-CONDUCTIVE
09C02-09	CONCRETE BASES, TYPES 1, 2, 5, & 6
09C03-04	TRANSFORMER/PEDESTAL BASES
09E05-06	TRAFFIC SIGNAL STANDARD ORNAMENTAL BRACKET MOUNTINGS TYPICAL FOR 13 FT. OR 15 FT.
09F15-04B	LOOP DETECTOR INSTALLED IN BASE COURSE WITH PULL (SPLICE) BOX OFF ROADWAY (OPTION 2)
13C19-03	HMA LONGITUDINAL JOINTS
15C04-05	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M.P.H. OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC
15C05-05	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M.P.H. OR LESS
15C07-15C	PAVEMENT MARKING ARROWS
15C08-23A	PERMANENT LONGITUDINAL PAVEMENT MARKINGS
15C08-23D	PAVEMENT MARKING (TURN LANES)
15C11-10B	CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS
15C19-08C	MOVING PAVEMENT MARKING OPERATION MULTI-LANE DIVIDED ROADWAY
15C33-04	STOP LINE AND CROSSWALK PAVEMENT MARKING
15D12-11A	TRAFFIC CONTROL, LANE CLOSURE
15D20-07A	TRAFFIC CONTROL, SINGLE LANE CLOSURE, DIVIDED NON-FREEWAY/EXPRESSWAY
15D20-07B	TRAFFIC CONTROL, SINGLE RIGHT LANE CLOSURE, UNDIVIDED NON-FREEWAY/EXPRESSWAY
15D20-07C	TRAFFIC CONTROL, SINGLE LEFT LANE CLOSURE, UNDIVIDED NON-FREEWAY/EXPRESSWAY
15D27-03	TRAFFIC CONTROL, SHOULDER CLOSURE ON DIVIDED ROADWAY, SPEEDS GREATER THAN 40 MPH
15D28-04	TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY
15D30-09A	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09B	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09C	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09D	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09F	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09K	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D39-02	TRAFFIC CONTROL, DROP-OFF SIGNING



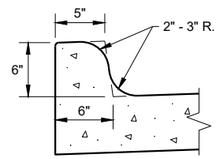
TYPES A (1) & D



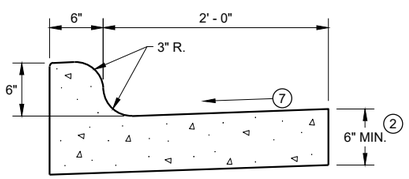
6" SLOPED CURB TYPES G (1) & J



4" SLOPED CURB TYPES G (1) & J

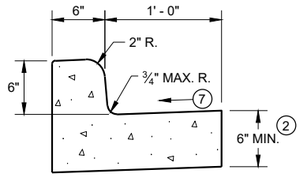


TYPES K (1) & L
(OPTIONAL CURB SHAPE)



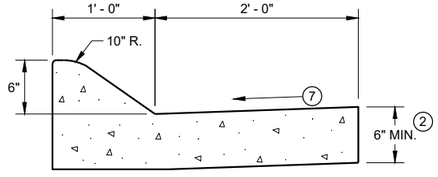
TYPES K (1) & L

CONCRETE CURB AND GUTTER 30"

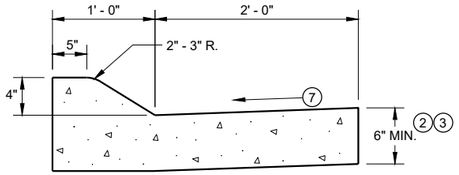


TYPES A (1) & D

CONCRETE CURB AND GUTTER 18"

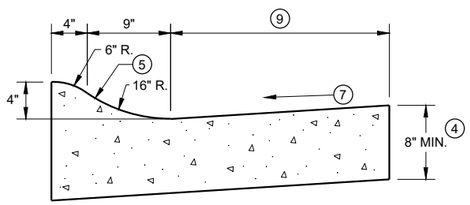


6" SLOPED CURB TYPES A (1) & D



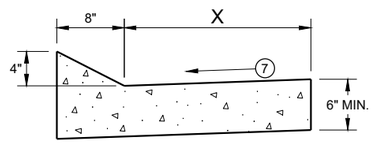
4" SLOPED CURB TYPES A (1) & D

CONCRETE CURB AND GUTTER 36"



4" SLOPED CURB TYPES R (1) & T

TBT & TBTT	X
30"	22"
36"	28"

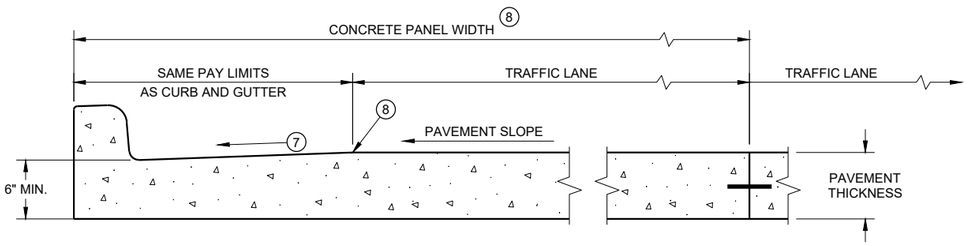


TYPES TBT & TBTT (1)

CONCRETE CURB AND GUTTER

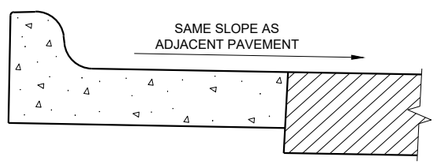
PAVEMENT THICKNESS AND MAXIMUM CONCRETE PANEL WIDTH TABLE

PAVEMENT THICKNESS	MAXIMUM PANEL WIDTH
LESS THAN 10"	12'
10" & ABOVE	15'



PARTIAL SECTION OF PAVEMENT * WITH INTEGRAL CURB AND GUTTER

* BIKE LANE IS NOT SHOWN



REVERSE SLOPE GUTTER (6)
(TYPICAL FOR ALL CURB & GUTTER TYPES)

GENERAL NOTES

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

INTEGRAL CURB AND GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB AND GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

(1) TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.

(2) THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.

(3) USE 8" MINIMUM GUTTER THICKNESS WHEN USED WITH AN ADJACENT CONCRETE TRUCK APRON PLACED BEHIND BACK OF CURB.

(4) THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.

(5) UNLESS OTHERWISE NOTED, FOR STAKING PURPOSES THE FACE OF CURB IS 6" FROM THE BACK OF CURB.

(6) WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.

(7) USE 4% GUTTER CROSS SLOPE UNLESS OTHERWISE NOTED IN THE PLANS.

(8) INCLUDE LONGITUDINAL JOINT AND TIE BARS ALONG LANE EDGE WHEN CONCRETE PANEL WIDTH EXCEEDS THE MAXIMUM WIDTH PER TABLE BELOW. LONGITUDINAL JOINT(S) ARE NOT ALLOWED WITHIN TRAFFIC LANES AND BIKE LANES. LONGITUDINAL JOINT MAY BE SAWED.

(9) CONCRETE CURB AND GUTTER 4-INCH SLOPED 30-INCH TYPE "R" AND "T" = 17 INCHES
CONCRETE CURB AND GUTTER 4-INCH SLOPED 36-INCH TYPE "R" AND "T" = 23 INCHES

CONCRETE CURB AND GUTTER

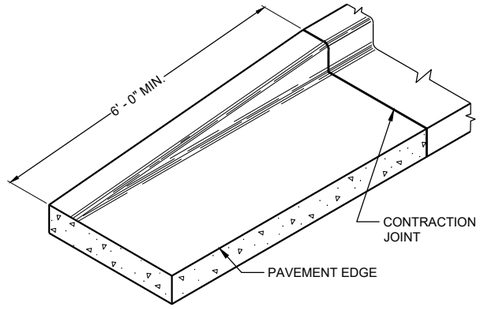
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

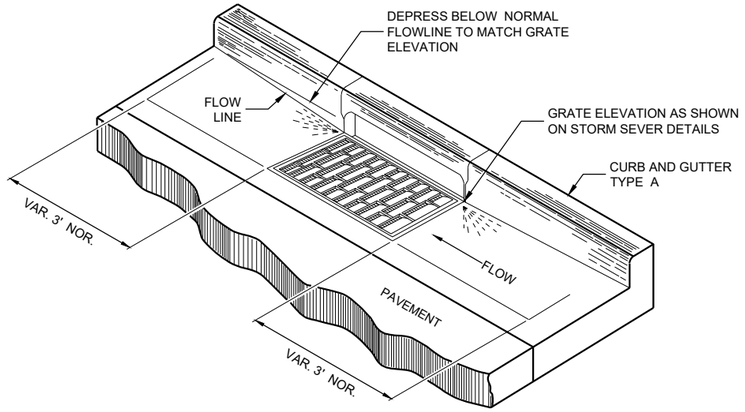
SDD 08D01 - 23a

6

SDD 08D01 - 23a



END SECTIONCURB AND GUTTER



DETAIL OF CURB AND GUTTER AT INLETS

(TYPICAL H INLET COVER SHOWN)

GENERAL NOTES

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

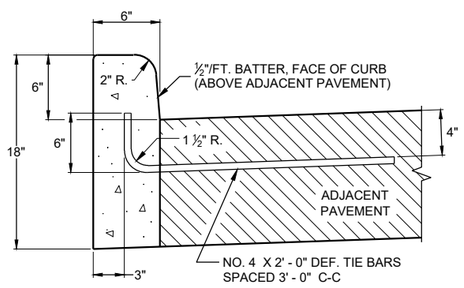
UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

(1) TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.

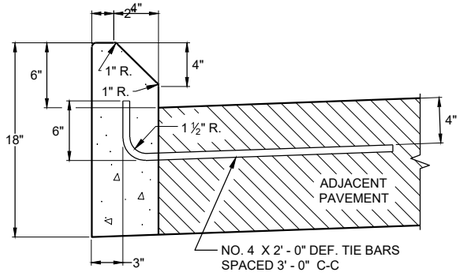
(2) THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.

(10) REFER TO SDD 08D18 AND 08D19 FOR ADDITIONAL DRIVEWAY ENTRANCE CURB DETAILS.

(11) PLACE 1" THICK EXPANSION JOINT MATERIAL BETWEEN VERTICAL FACE CURB TYPES EXTENDING FROM THE TOP OF CURB TO 1 INCH BELOW THE ADJOINING CONCRETE SURFACE. RIGID CONCRETE STRUCTURES INCLUDE RAISED CONCRETE MEDIANS, CONCRETE SAFETY ISLANDS, SPLITTER ISLANDS, OR LOCATIONS IDENTIFIED ON THE PLANS.

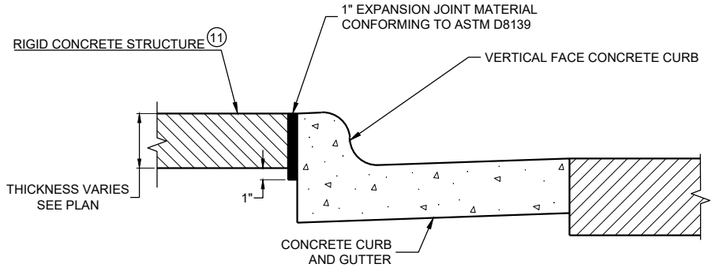


TYPES A (1) & D

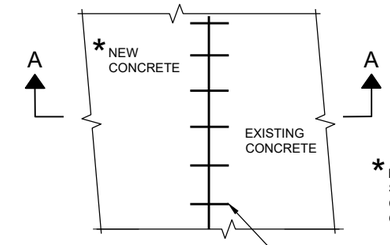


TYPES G (1) & J

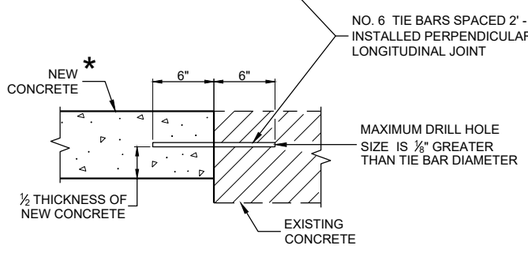
CONCRETE CURB



EXPANSION JOINT DETAIL FOR VERTICAL CURB ABUTTING A RIGID STRUCTURE (11)

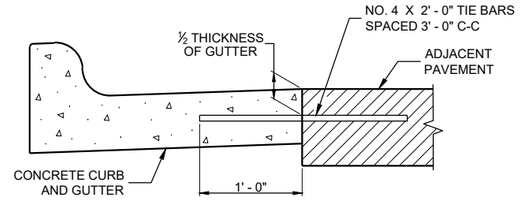


PLAN VIEW

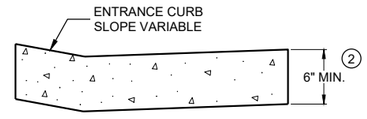


SECTION A - A

TIE BARS DRILLED INTO EXISTING PAVEMENT



TYPICAL TIE BAR LOCATION (1)



DRIVEWAY ENTRANCE CURB (10)

(WHEN DIRECTED BY THE ENGINEER)

CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 — DATE
/S/ Rodnery Taylor — ROADWAY STANDARDS DEVE ENGINEER

FHWA

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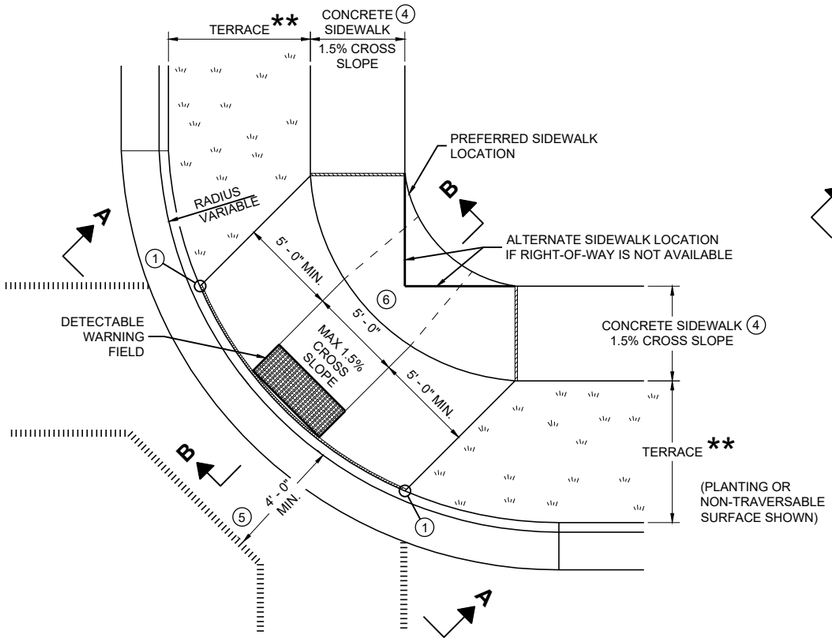
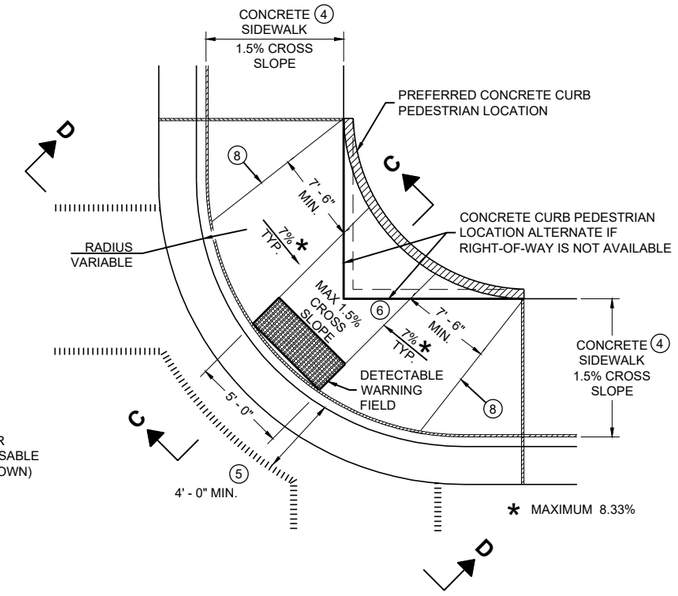
SDD08D01 - 23b



PLAN VIEW

CURB RAMP TYPE 1

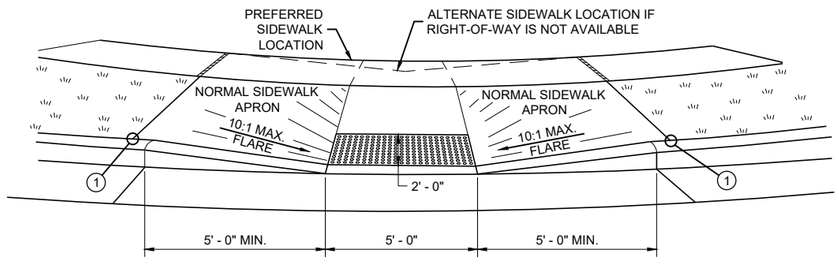
(CENTER OF CORNER RADIUS)



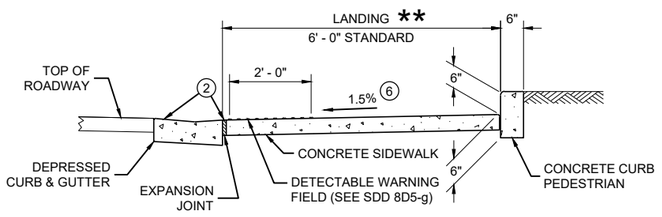
PLAN VIEW

CURB RAMP TYPE 1 - A

(NO TERRACE)

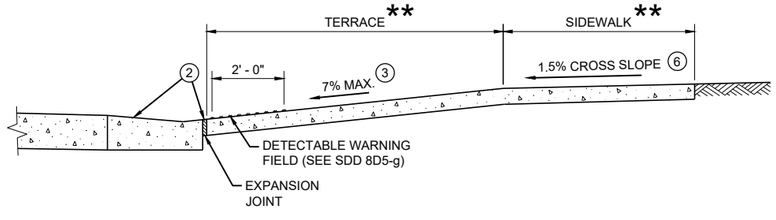


VIEW A - A FOR TYPE 1

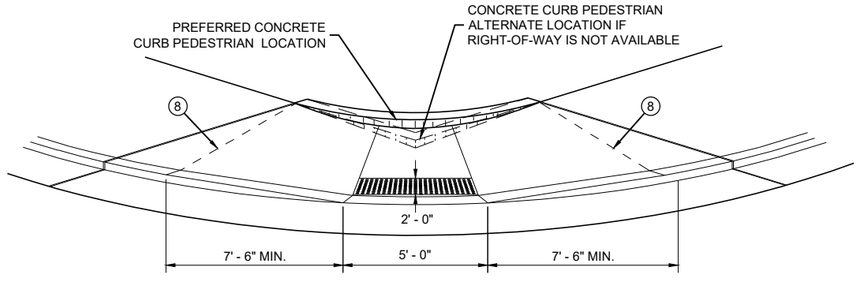


SECTION C - C FOR TYPE 1 - A

** WIDTH SHOWN ELSEWHERE IN THE PLANS



SECTION B - B FOR TYPE 1



VIEW D - D FOR TYPE 1 - A

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

WHEN NECESSARY, THE SIDEWALK ELEVATION MAY BE LOWERED TO MEET THE HIGH POINT ON THE RAMP.

TYPE 1 CURB RAMPS SHALL HAVE A NORMAL SIDEWALK APRON AND CURB ON BOTH SIDES OF RAMP.

DETECTABLE WARNING FILED SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS "CURB RAMP DETECTABLE WARNING FIELD". THE CONCRETE PEDESTRIAN CURB, IF NEEDED, SHALL BE MEASURED AND PAID BY THE LINEAR FOOT AS "CONCRETE CURB PEDESTRIAN". CONCRETE SIDEWALK IN THE CURB RAMP AREA SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS CONCRETE SIDEWALK, INCLUDING THE AREA UNDER THE DETECTABLE WARNING FIELD.

SELECT CURB RAMP DETECTABLE WARNING FIELD MATERIALS AND DEVICES FROM THE DEPARTMENT'S APPROVED MATERIALS LIST. THE COLOR OF THE DETECTABLE WARNING FIELD IS SPECIFIED ELSEWHERE AND IS INCIDENTAL TO THE BID ITEM OF "CURB RAMP DETECTABLE WARNING FIELD"

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

SURFACE TEXTURE OF THE RAMP SHALL BE OBTAINED BY COARSE BROOMING TRANSVERSE TO THE SLOPE OF THE RAMP.

(1) THIS POINT IS AN EXTENSION OF OUTSIDE EDGE OF APPROACHING SIDEWALK WHERE IT MEETS THE BACK OF CONCRETE CURB. POINT LOCATION MAY BE ADJUSTED TO ALIGN WITH BEGINNING OF FULL-HEIGHT CURB IF THIS DISTANCE IS SHORT.

(2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN ¼ - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.

(3) MAXIMUM 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.

(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

(5) PROVIDE A LEVEL LANDING IN THE STREET AND GUTTER AREA (2% MAXIMUM SLOPE IN ANY DIRECTION). WHEN THE GUTTER SLOPE EXCEEDS 2%, CONSTRUCT THE LEVEL LANDING IN THE STREET AREA. 4 FOOT WIDTH IS MEASURED FROM THE FLANGE LINE

(6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.

(8) PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

LEGEND

- ½" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)

CURB RAMPS TYPE 1 AND 1-A

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

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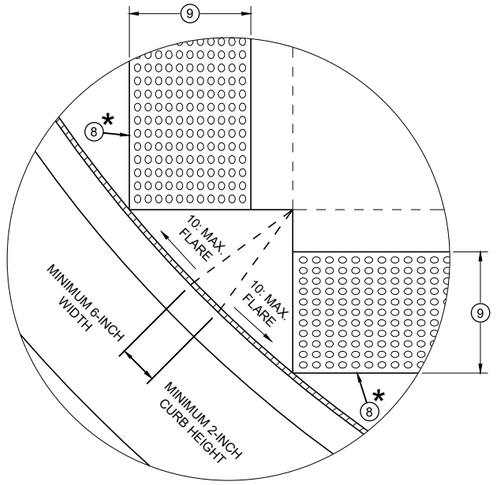
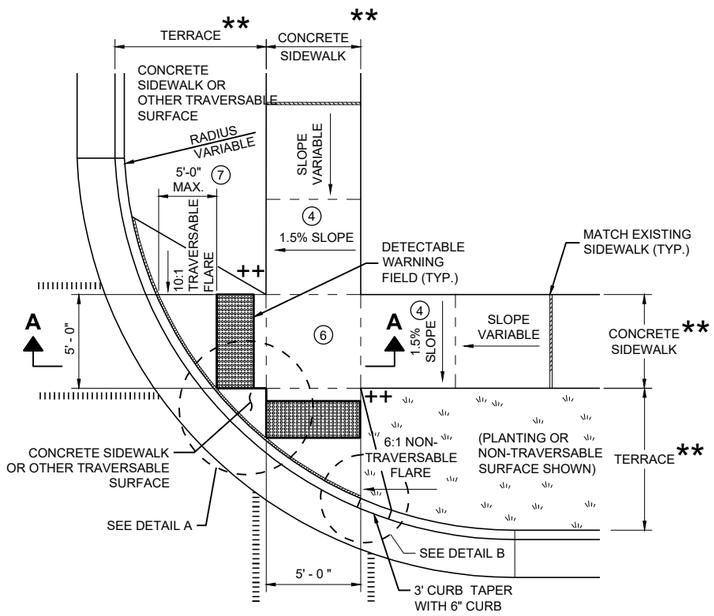
SDD 08D05-21a



DETAIL A



PLAN VIEW
CURB RAMP TYPE 2
(CENTER OF CORNER RADIUS)

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE SHALL BE FROM THE SAME MANUFACTURER.

(2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN ¼ - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.

(3) AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE (2.67% OR LESS) AND NOT TO EXCEED 11% GRADE CHANGE.

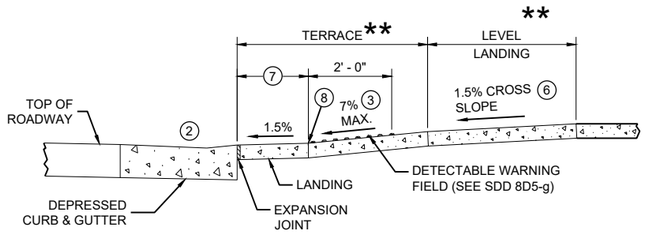
(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

(6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET X 5 FEET.

(7) WHEN GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.

(8) PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

(9) WHEN DISTANCE IS LESS THAN 6' - 0", IT MAY BE DIFFICULT TO ACHIEVE A 7% SLOPE OR FLATTER ALONG THE RAMP. REDUCE CURB HEIGHT IN TRIANGLE AREA TO ACHIEVE 7% SLOPE OR FLATTER ON RAMP. CONSTRUCT 2-INCH MINIMUM CURB HEIGHT BETWEEN 10:1 FLARES.

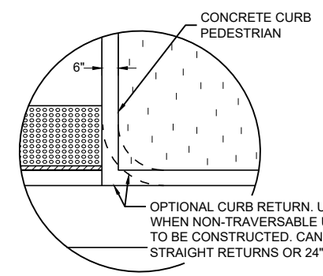


SECTION A - A FOR TYPE 2

* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK

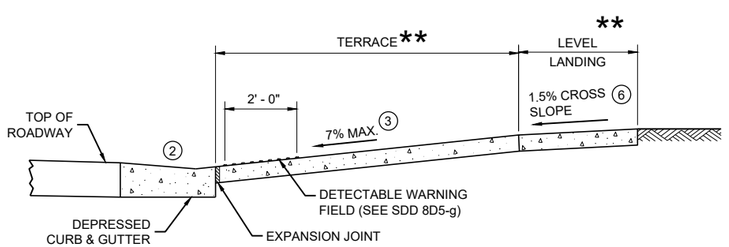
** WIDTH SHOWN ELSEWHERE IN THE PLANS

++ CONSTRUCT 6" WEDGE TO AVOID CONCRETE BREAKAGE



DETAIL B

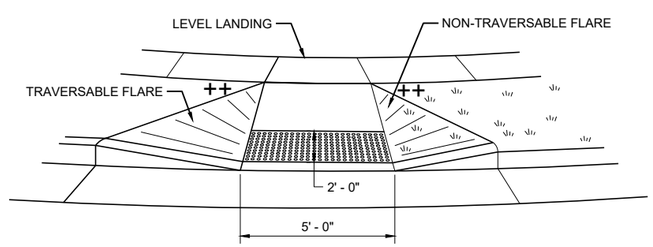
PLAN VIEW
CURB RAMP TYPE 3
(OUTSIDE OF CROSSWALK AREA)



SECTION B - B FOR TYPE 3

LEGEND

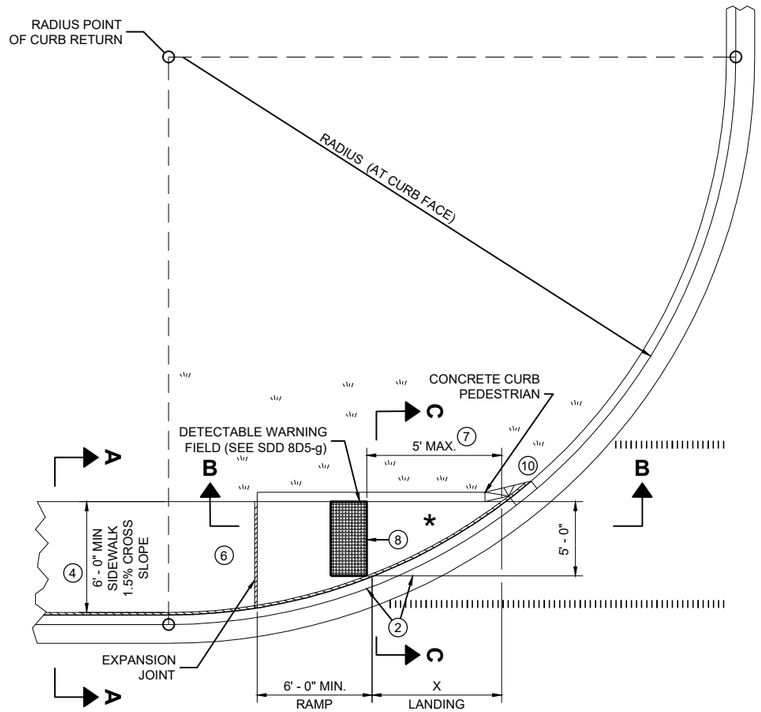
- ½" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT SIDEWALK
- PAVEMENT MARKING CROSSWALK (WHITE)



VIEW C - C FOR TYPE 3

CURB RAMPS TYPE 2 AND 3

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

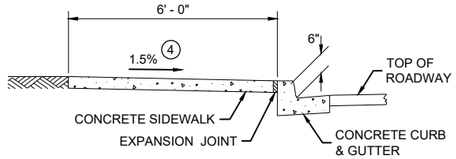


PLAN VIEW

CURB RAMP TYPE 4A

RADIUS (AT CURB FACE)	X
10 FEET	4' - 7"

INTERMEDIATE RADII CAN BE INTERPOLATED



SECTION A - A FOR TYPE 4A

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

(2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN ¼ - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.

(3) AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.

(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

(6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.

(7) WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.

(8) PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

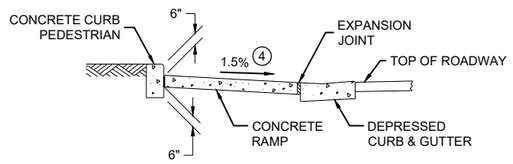
(10) INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

LEGEND

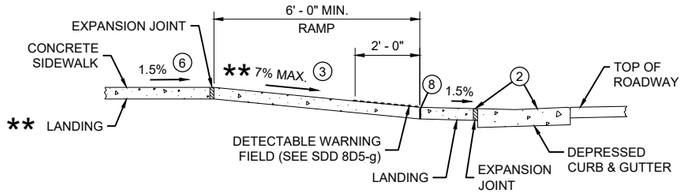
½" EXPANSION JOINT SIDEWALK

CONTRACTION JOINT SIDEWALK

PAVEMENT MARKING CROSSWALK (WHITE)

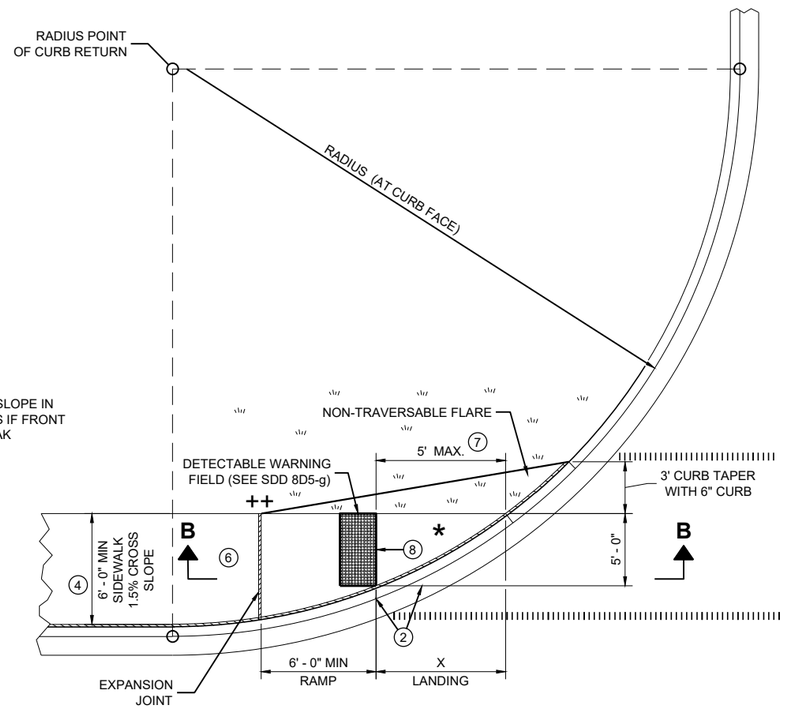


SECTION C - C FOR TYPE 4A



SECTION B - B FOR TYPE 4A AND TYPE 4A1

** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

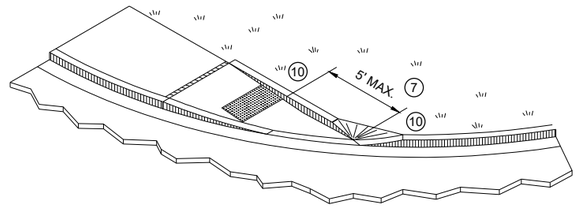


* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IF FRONT OF GRADE BREAK

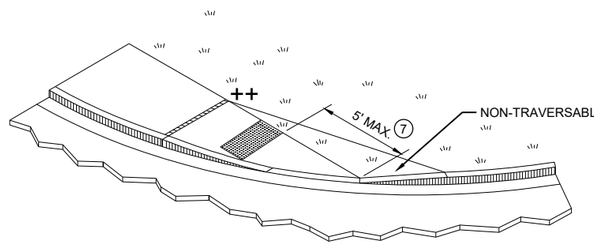
PLAN VIEW

CURB RAMP TYPE 4A1

++ CONSTRUCT 6" WEDGE TO AVOID CONCRETE BREAKAGE



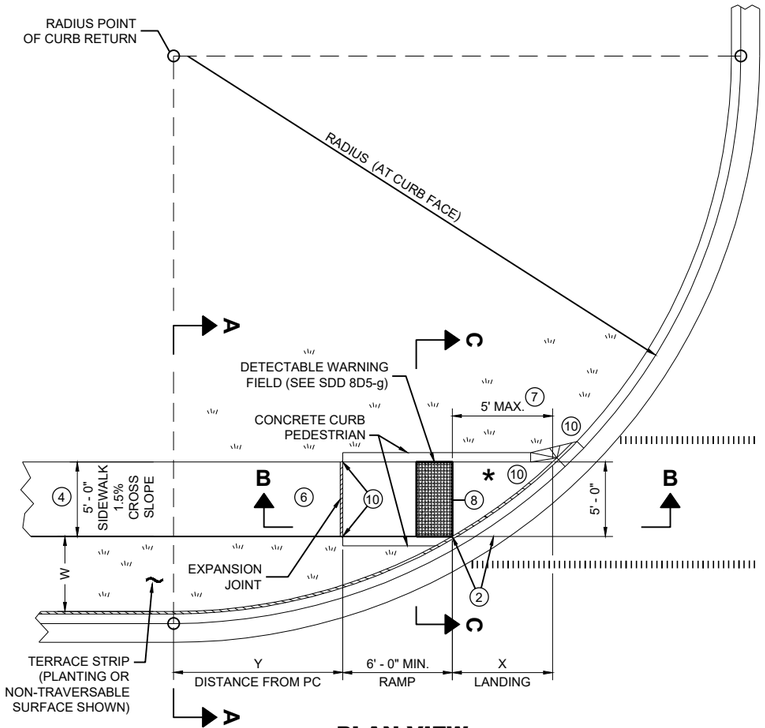
ISOMETRIC VIEW FOR TYPE 4A



ISOMETRIC VIEW FOR TYPE 4A1

CURB RAMPS
TYPE 4A AND 4A1

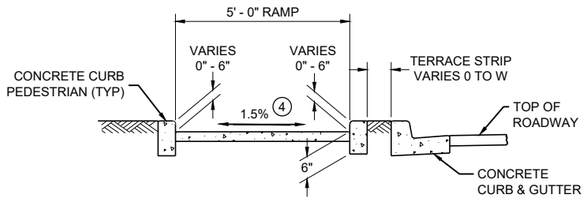
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



PLAN VIEW
CURB RAMP TYPE 4B

RADIUS (AT CURB FACE)	W = 3' - 0"		W = 4' - 0"		W = 5' - 0"		W = 6' - 0"		W = 7' - 0"		W = 8' - 0"		W = 9' - 0"		W = 10' - 0"	
	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y
10 FEET	2' - 10 1/4"	0' - 5"	2' - 1"	1' - 4 1/2"	1' - 5"	2' - 1"	0' - 10"	2' - 7 1/2"	0' - 3 1/4"	3' - 0 1/4"						
15 FEET	4' - 6 3/4"	2' - 1 3/4"	3' - 9"	3' - 5 3/4"	3' - 1 1/4"	4' - 6"	2' - 6 3/4"	5' - 4 1/2"	2' - 1"	6' - 1"	1' - 8"	6' - 8 1/2"	1' - 3 1/4"	7' - 2 1/2"	0' - 10 3/4"	7' - 7 1/4"
20 FEET			4' - 11 1/2"	5' - 1 3/4"	4' - 3 1/4"	6' - 5 1/2"	3' - 8 3/4"	7' - 7"	3' - 3"	8' - 6 1/2"	2' - 10"	9' - 4 1/2"	2' - 5 1/2"	10' - 1 1/4"	2' - 1 1/4"	10' - 9"
30 FEET									4' - 10 3/4"	12' - 5 3/4"	4' - 5 1/2"	13' - 7 3/4"	4' - 0 3/4"	14' - 8 1/2"	3' - 8 1/2"	15' - 8 1/4"
40 FEET															4' - 10 3/4"	19' - 8 1/4"

INTERMEDIATE RADII CAN BE INTERPOLATED
DIMENSION "Y" IS CALCULATED BASED ON 6'-0" RAMP LENGTH
DIMENSION "X" IS CALCULATED BASED ON 5'-0" SIDEWALK WIDTH



SECTION C - C FOR TYPE 4B

LEGEND

- ½" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT SIDEWALK
- PAVEMENT MARKING CROSSWALK (WHITE)

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

(2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN ¼ - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.

(3) AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.

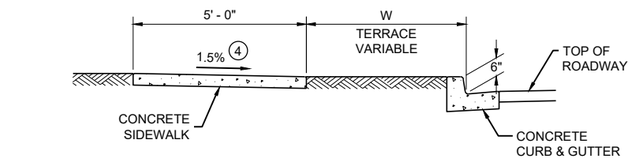
(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

(6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.

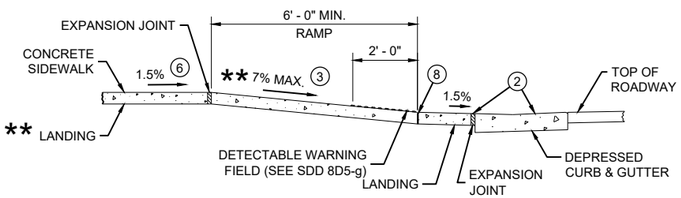
(7) WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.

(8) PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

(10) INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

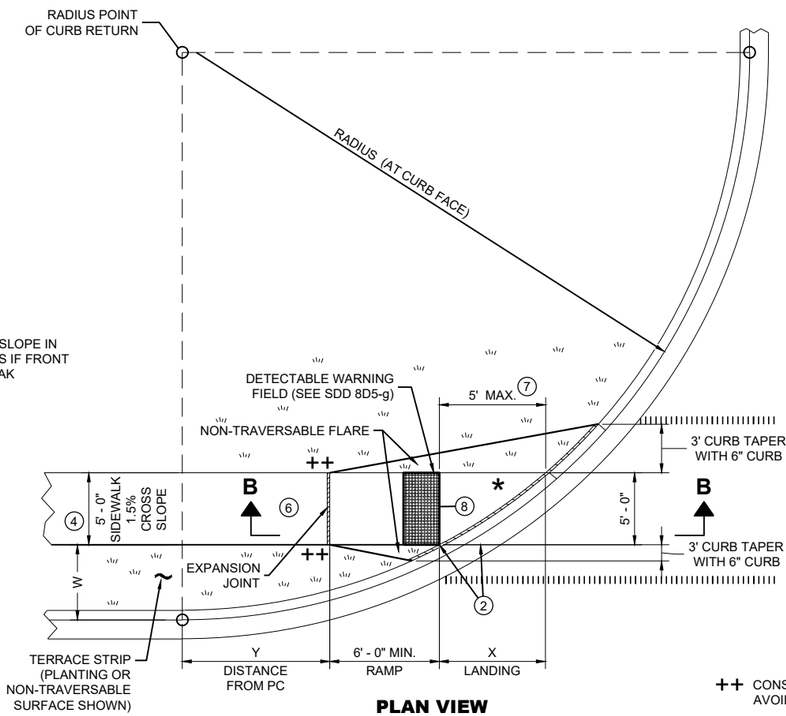


SECTION A - A FOR TYPE 4B



SECTION B - B FOR TYPE 4B AND TYPE 4B1

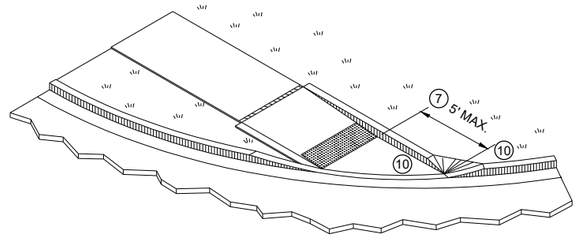
** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED



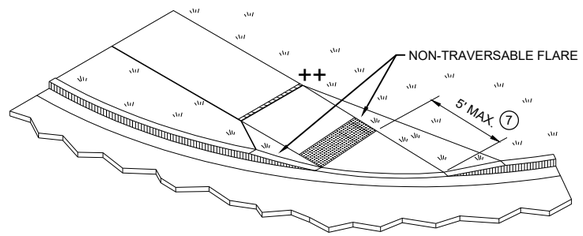
PLAN VIEW
CURB RAMP TYPE 4B1

* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IF FRONT OF GRADE BREAK

++ CONSTRUCT 6" WEDGE TO AVOID CONCRETE BREAKAGE



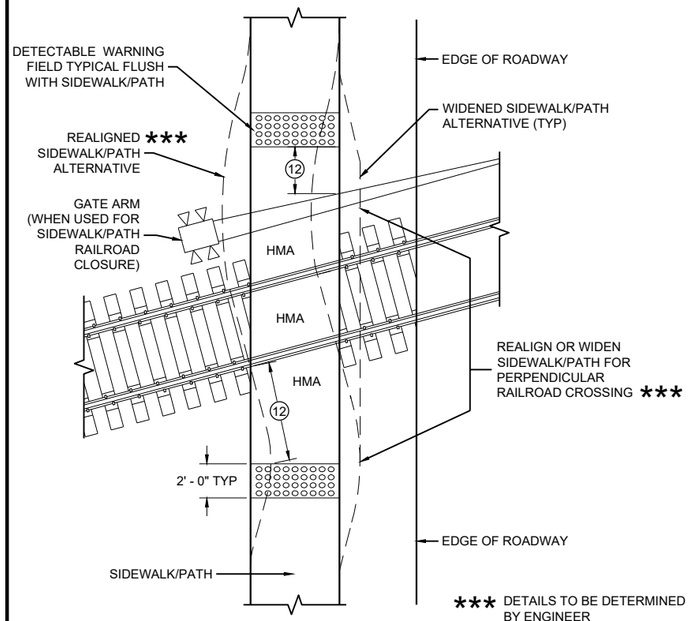
ISOMETRIC VIEW FOR TYPE 4B



ISOMETRIC VIEW FOR TYPE 4B1

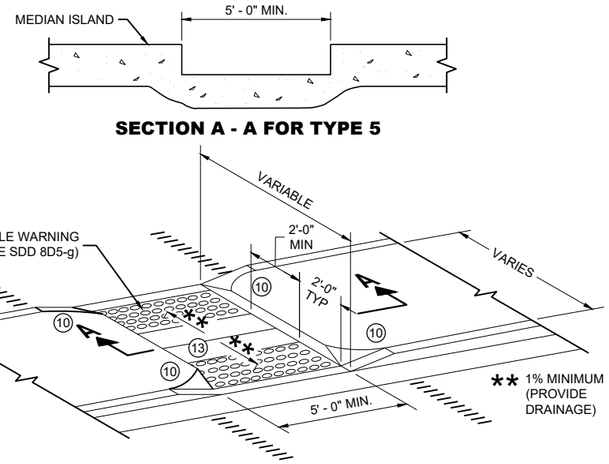
CURB RAMPS
TYPE 4B AND 4B1

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



CURB RAMP TYPE 8

DETECTABLE WARNINGS FOR SIDEWALKS OR SHARED USE PATHS AT RAILROAD CROSSINGS



SECTION A - A FOR TYPE 5

CURB RAMP TYPE 5

MEDIAN ISLAND NON-ELEVATED PEDESTRIAN CROSSING

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

(2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN ¼ - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.

(3) AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.

(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

(6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.

(10) INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

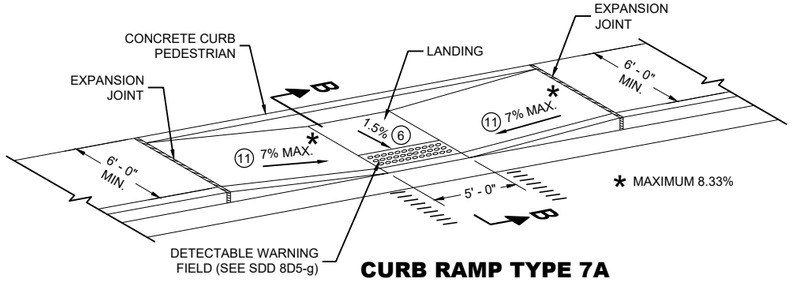
(11) SLOPE SIDEWALK TOWARD LANDING AS SHOWN WHERE THERE IS NO TERRACE OR WHERE THE TERRACE WIDTH IS LESS THAN 6 FEET WIDE.

(12) THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO A RAILROAD CROSSING SHALL BE 1.5 FEET ±0.1' FROM THE FACE OF THE GATE ARM IF THE GATE ARM EXTENDS ACROSS THE SIDEWALK/PATH. WHERE THERE IS NO PEDESTRIAN GATE, THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO THE RAILROAD TRACK IS 15 FEET MAXIMUM AND 12 FEET MINIMUM, 15 FEET TYPICAL FROM THE NEAREST RAIL.

(13) DO NOT INSTALL DETECTABLE WARNING FIELDS AT THE EDGES OF STEET-LEVEL PEDESTRIAN REFUGE ISLANDS IF A MINIMUM 2 FOOT CONCRETE SURFACE WITHOUT DETECTABLE WARNINGS (MEASURED IN THE DIRECTION OF PEDESTRIAN TRAVEL) CANNOT BE ACHIEVED.

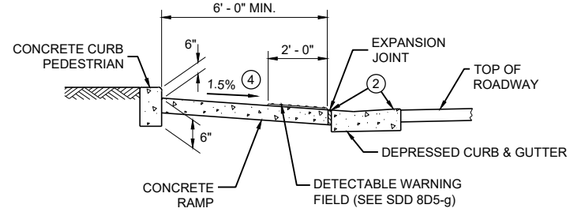
LEGEND

- ½" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)

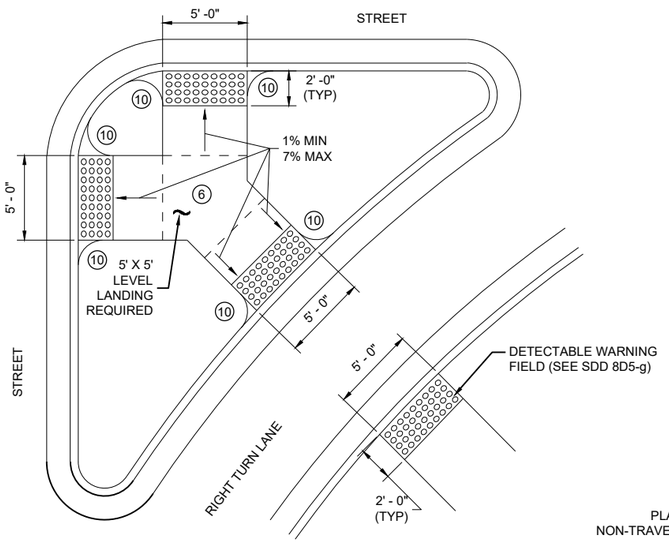


CURB RAMP TYPE 7A

FOR INTERSECTIONS AND MID BLOCK CROSSINGS



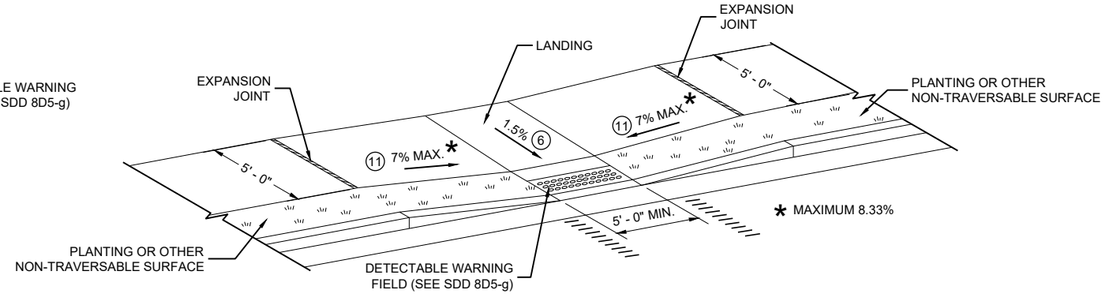
SECTION B - B FOR TYPE 7A



CURB RAMP TYPE 6

DETECTABLE WARNING AT ISLANDS

REFER TO GENERAL NOTES (2) AND (3) FOR ALL ISLAND CURB RAMPS



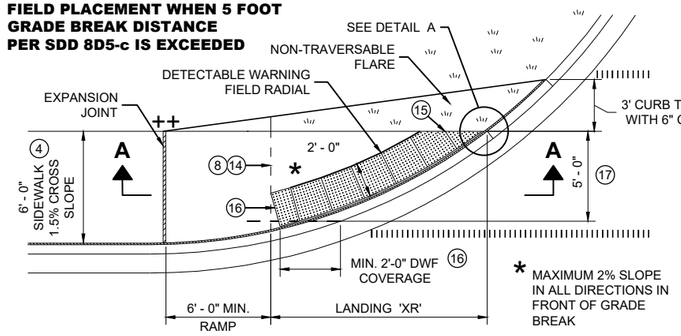
CURB RAMP TYPE 7B

FOR INTERSECTIONS AND MID BLOCK CROSSINGS

CURB RAMPS TYPE 5, 6, 7A, 7B & 8

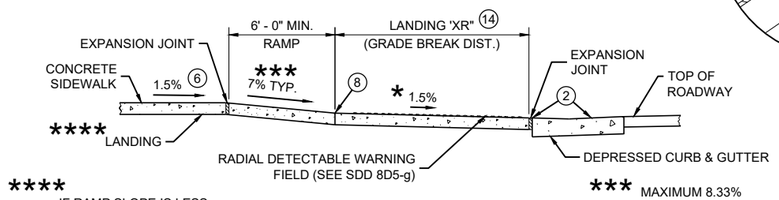
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-c IS EXCEEDED



PLAN VIEW

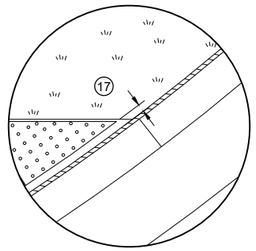
CURB RAMP TYPE 4A1
(GRADE BREAK DISTANCE GREATER THAN 5 FEET)



**** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

*** MAXIMUM 8.33%

SECTION A - A FOR TYPE 4A1



DETAIL A

LEGEND

- ½" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT SIDEWALK
- PAVEMENT MARKING CROSSWALK (WHITE)

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

APPLY RADIAL DETECTABLE WARNING PLACEMENT SIMILARLY FOR TYPE 4A AND 4A1 CURB RAMPS AND SIMILARLY FOR TYPE 4B AND 4B1 CURB RAMPS. TYPE 4A AND 4B CURB RAMPS ARE NOT SHOWN.

REFER TO SDD 8D5-g FOR ADDITIONAL RADIAL PLATE REQUIREMENTS.

FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FILED ARE PROHIBITED.

DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AD ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.

(2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN ¼ - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.

(3) AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.

(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

(6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET BY 5 FEET.

(8) PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

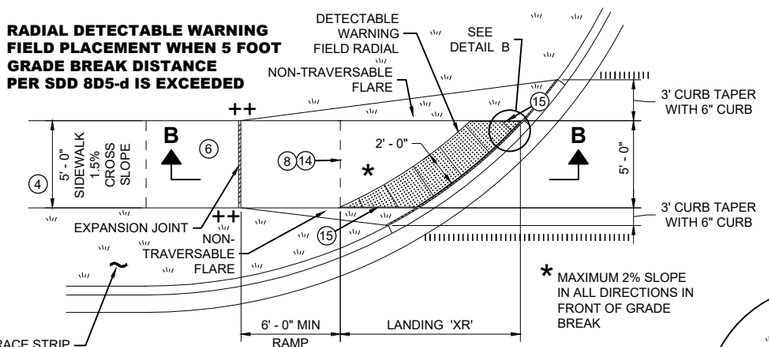
(14) CONSULT ENGINEER IF GRADE BREAK LOCATION (END OF LANDING DIMENSION "XR") REQUIRES FIELD ADJUSTMENT WHEN ESTABLISHING FINAL RADIAL DETECTABLE WARNING FIELD LOCATION.

(15) FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN ⅛" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.

(16) USE 1' X 2" RECTANGULAR END PLATE AT END OF TYPE 4A1 RAMP AND PROVIDE MINIMUM 2' - 0" DETECTABLE WARNING FIELD COVERAGE (IN DIRECTION OF PEDESTRIAN TRAVEL) ALONG THE ENTIRE CURB RAMP WIDTH.

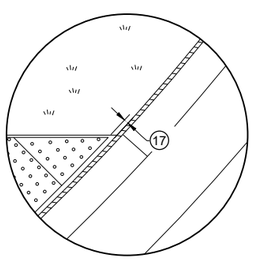
(17) A MAXIMUM 3 INCH CONCRETE BORDER WIDTH IS ALLOWABLE IN FRONT OF RADIAL DETECTABLE WARNING FIELD FOR CONSTRUCTABILITY PURPOSES. CONCRETE BORDER WIDTH MAY VARY UP TO 1 INCH.

RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-d IS EXCEEDED

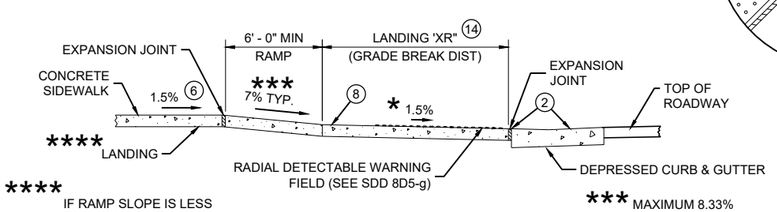


PLAN VIEW

CURB RAMP TYPE 4B1
(GRADE BREAK DISTANCE GREATER THAN 5 FEET)



DETAIL B

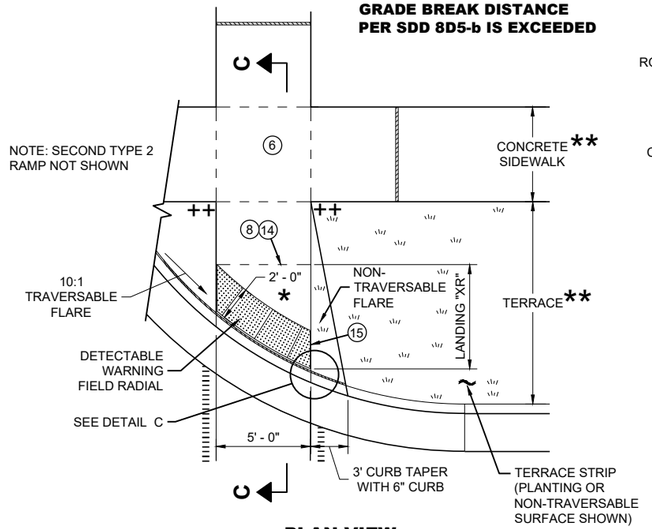


**** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

*** MAXIMUM 8.33%

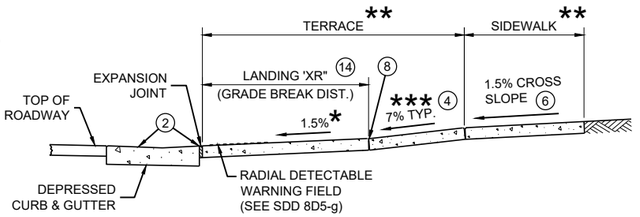
SECTION B - B FOR TYPE 4B1

RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-b IS EXCEEDED

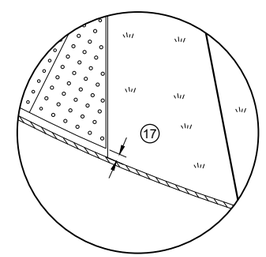


PLAN VIEW

CURB RAMP TYPE 2
(GRADE BREAK DISTANCE GREATER THAN 5 FEET)
(ON LINE WITH SIDEWALK)



SECTION C - C FOR TYPE 2



DETAIL C

* MAXIMUM 2% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK

** WIDTH SHOWN ELSEWHERE IN THE PLANS

*** MAXIMUM 8.33%

++ CONSTRUCT 6" WEDGE TO AVOID CONCRETE BREAKAGE

CURB RAMPS
RADIAL DETECTABLE WARNING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

SDD 08D05-21f

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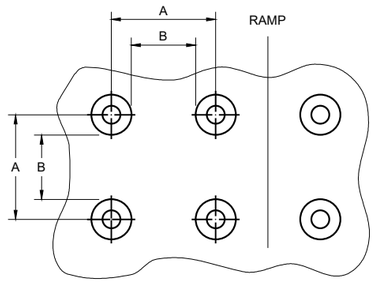
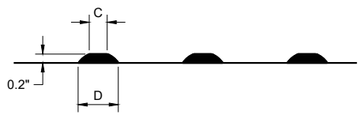
SDD 08D05-21f

	MIN.	MAX.
A	1.6"	2.4"
B	0.65"	1.5"
C	*	*
D	0.9"	1.4"

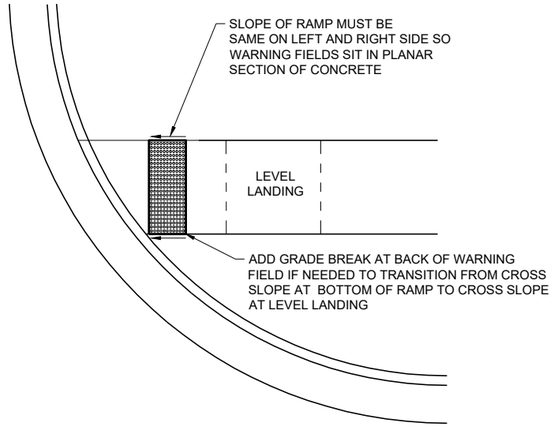
* THE C DIMENSION IS 50% TO 65% OF THE D DIMENSION.



PLAN VIEW



ELEVATION VIEW



DETECTABLE WARNING FIELD
PLANAR INSTALLATION

GENERAL NOTES

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AT A CURB RAMP SHALL BE FROM THE SAME MANUFACTURER.

PLACE ALL DETECTABLE WARNING FIELD SYSTEMS IN ACCORDANCE TO THE MANUFACTURER'S RECOMMENDATION.

FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FILED ARE PROHIBITED.

DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.

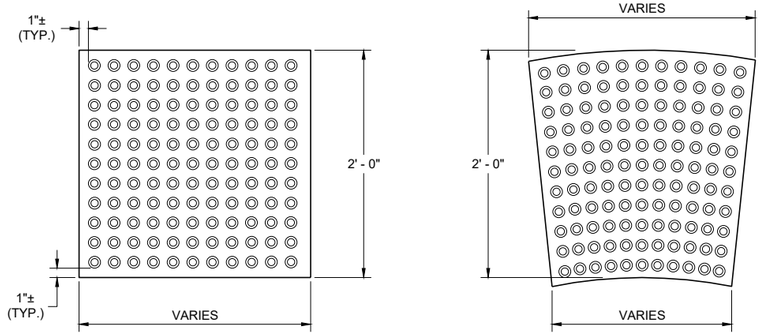
FOR RADIAL DETECTABLE WARNING FIELD APPLICATIONS WHERE STANDARD RADIAL PLATES ARE NOT AVAILABLE AT AN INTERSECTION CURB RADIUS, A COMBINATION OF SQUARE OR RECTANGULAR PLATES AND RADIAL PLATES MAY BE USED TO FORM RADIAL CONFIGURATION. RADIAL WEDGE PLATES IN COMBINATION WITH SQUARE PLATES ARE ALSO ACCEPTABLE. FOLLOW MANUFACTURER'S RECOMMENDATIONS.

REFER TO CONTRACT AND STANDARD SPECIFICATIONS FOR FIELD CUTTING REQUIREMENTS.

DO NOT EMBED IN CONCRETE ANY FIELD-CUT PLATES WITH CUT EDGES SHORTER THAN 6 INCHES. CONSULT WITH MANUFACTURER FOR RE-DRILLING AND ANCHORING REQUIREMENTS OF FIELD-CUT PLATES.

(15) FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN ⅛" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.

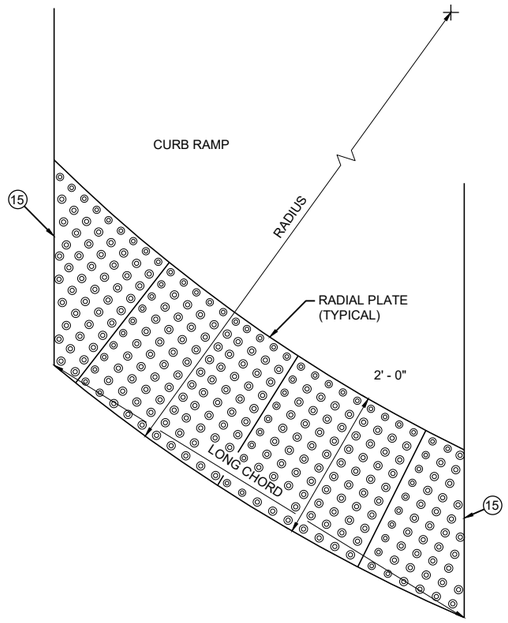
TRUNCATED DOMES
DETECTABLE WARNING PATTERN DETAIL



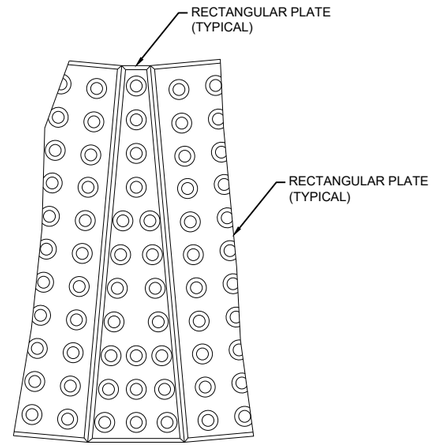
RECTANGULAR PLATES

RADIAL PLATES

PLAN VIEW
DETECTABLE WARNING FIELDS (TYPICAL)



PLAN VIEW
RADIAL DETECTABLE WARNING FIELD ATTRIBUTES



PLAN VIEW
RADIAL WEDGE PLATE CONNECTION DETAIL

CURB RAMPS
RECTANGULAR AND RADIAL DETECTABLE WARNING PLATES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

July 2023 — DATE

/S/ Rodney Taylor — ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR

FHWA

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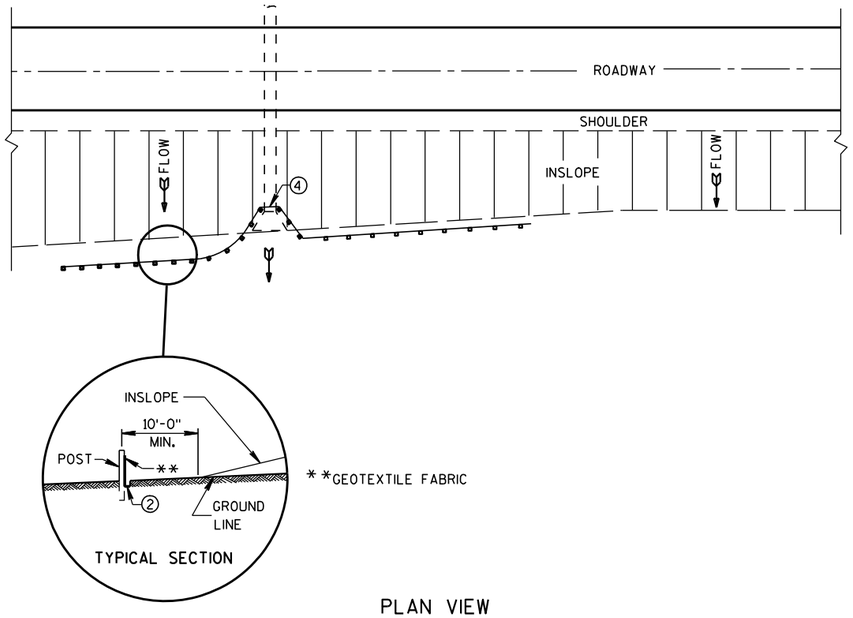
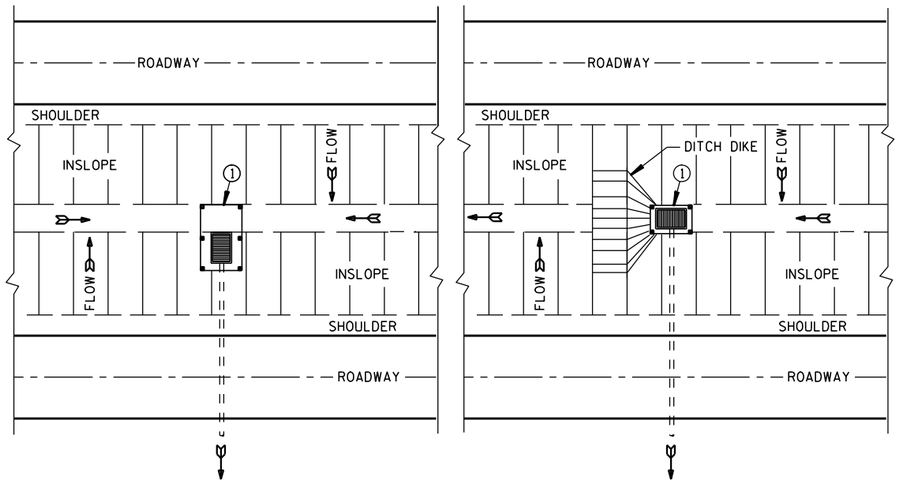
SDD 08D05-21g



PLAN VIEW

TYPICAL APPLICATION OF SILT FENCE



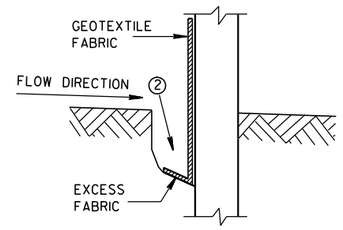
PLAN VIEW

SILT FENCE AT MEDIAN SURFACE DRAINS

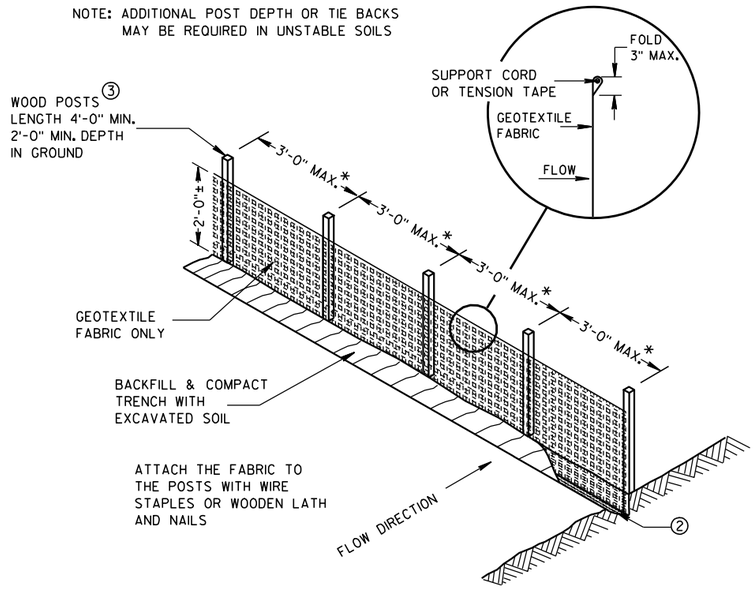
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

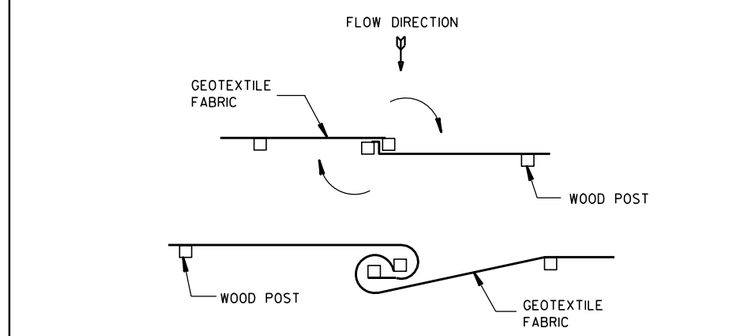
1. HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
2. FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
3. WOOD POSTS SHALL BE A MINIMUM SIZE OF 1⅛" X 1⅛" OF OAK OR HICKORY.
4. SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
5. CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



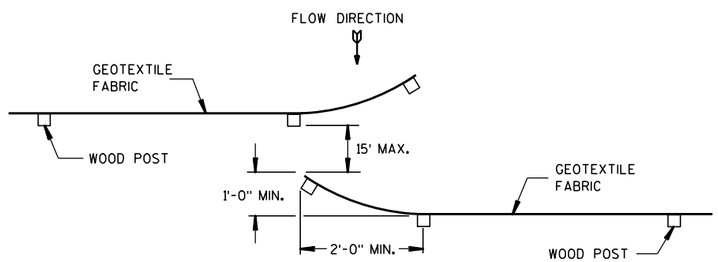
TRENCH DETAIL



SILT FENCE

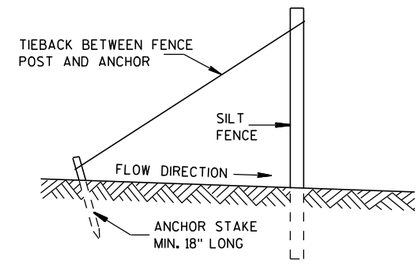


TWIST METHOD



HOOK METHOD

JOINING TWO LENGTHS OF SILT FENCE 5



SILT FENCE TIE BACK

(WHEN REQUIRED BY THE ENGINEER)

SILT FENCE	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED	
4-29-05 DATE	/S/ Beth Cannestra CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA	

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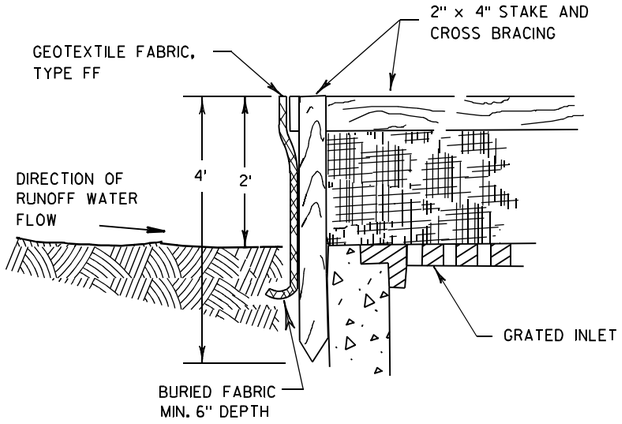
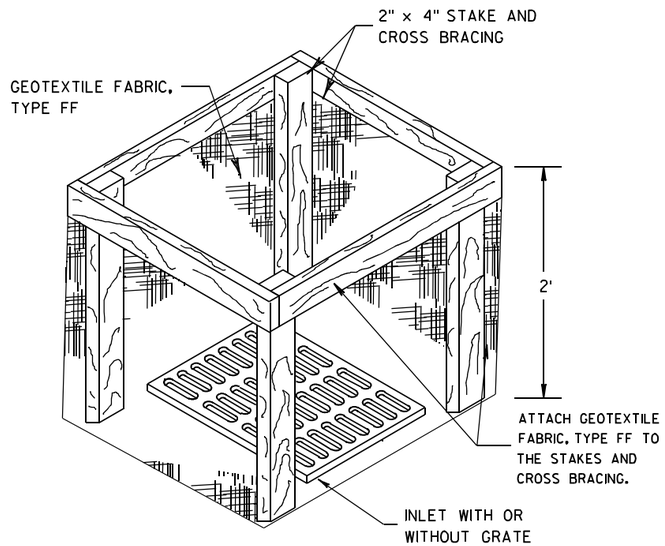
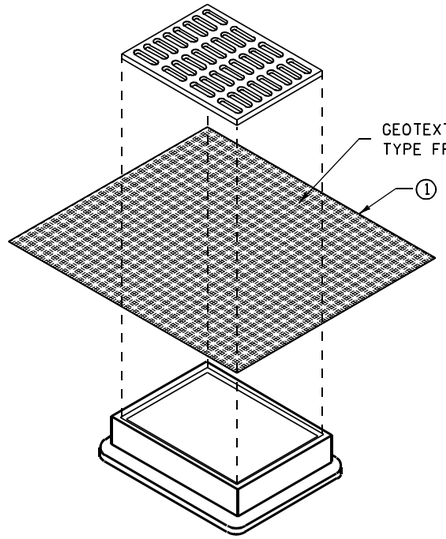
S.D.D. 8 E 9-6



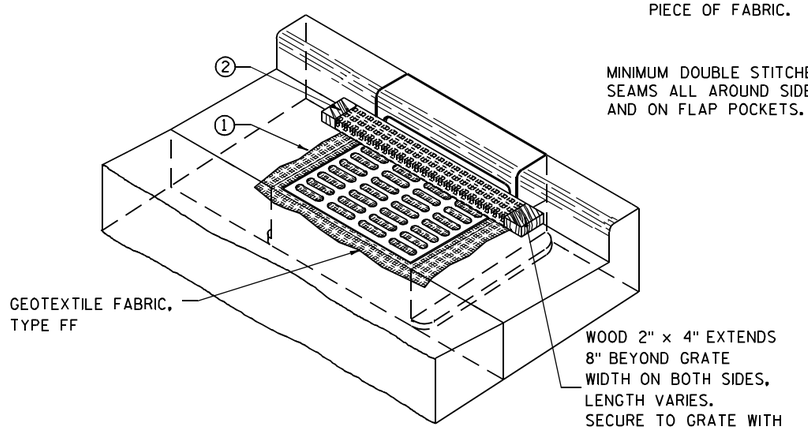


INLET PROTECTION, TYPE A

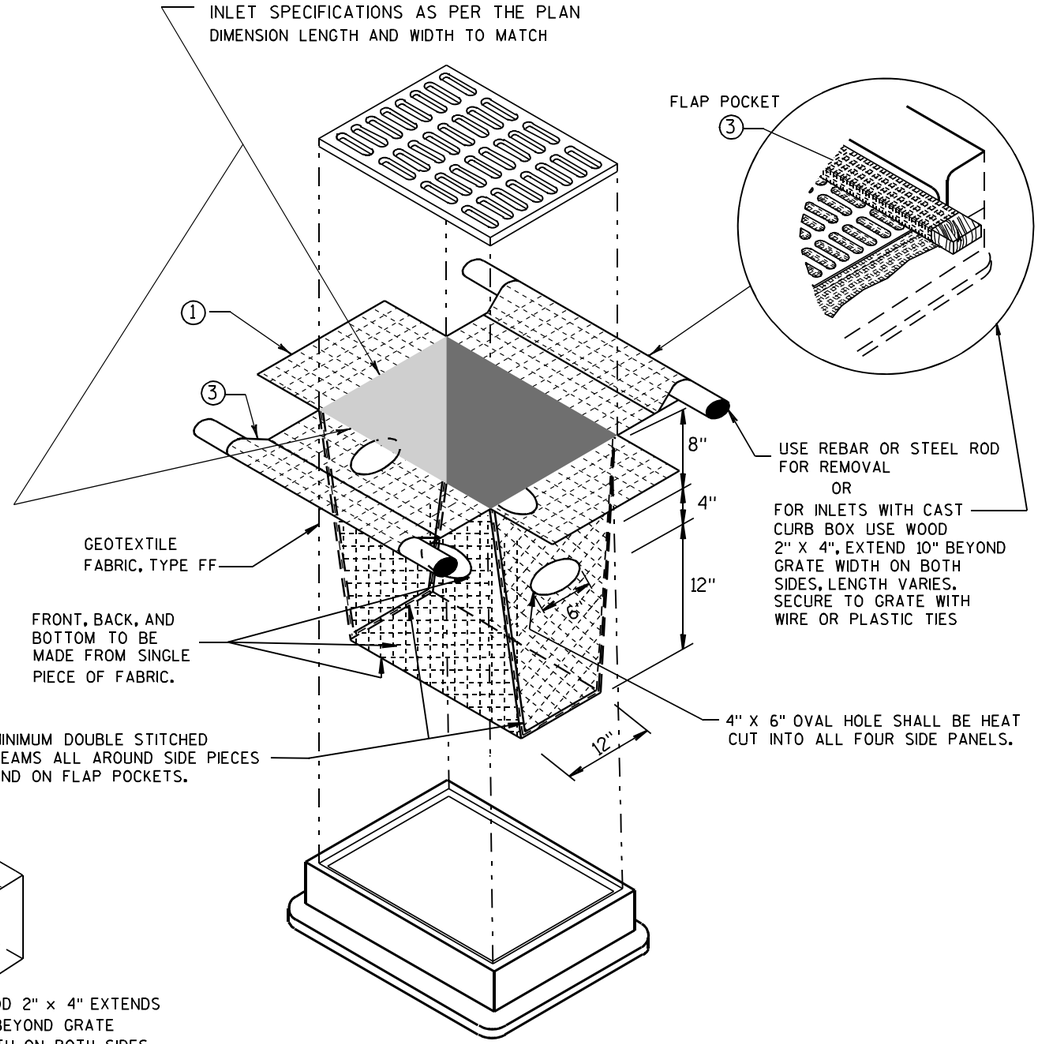


INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE C (WITH CURB BOX)



INLET PROTECTION, TYPE D

(CAN BE INSTALLED IN ANY INLET TYPE WITH
OR WITHOUT A CURB BOX AS PER NOTE ②)

GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.

② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.

③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.

INLET PROTECTION
TYPE A, B, C, AND D

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
10/16/02
DATE
/S/ Beth Cannestra
CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA

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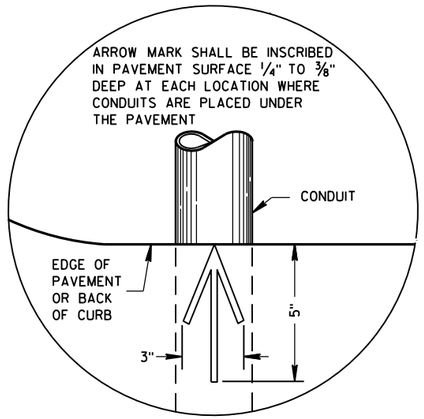
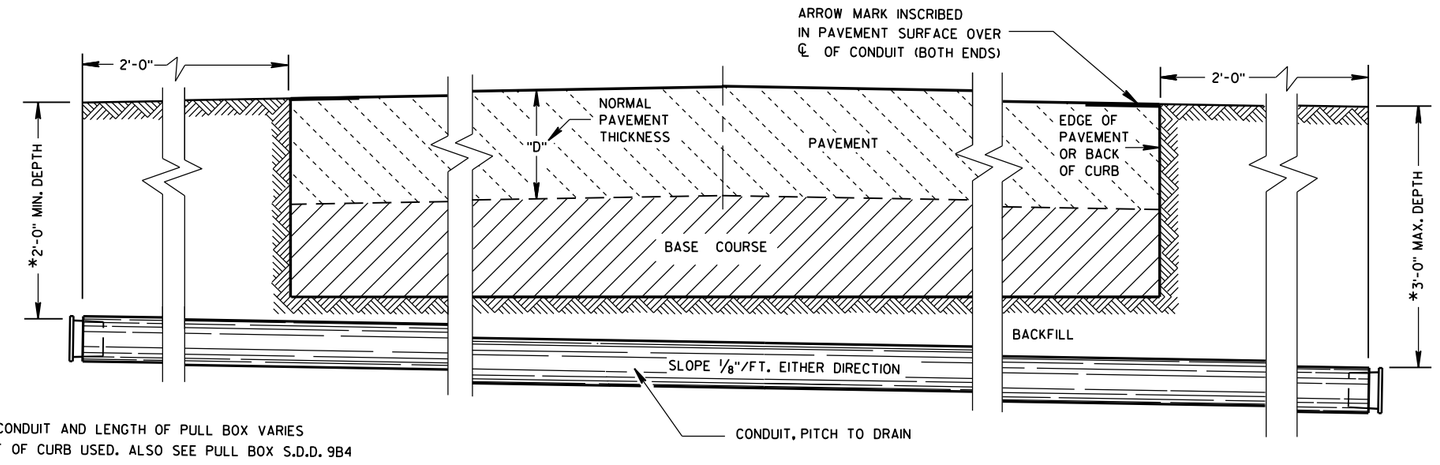
S.D.D. 8 E 10-2



PLAN VIEW

ARROW MARK



*DEPTH OF CONDUIT AND LENGTH OF PULL BOX VARIES WITH HEIGHT OF CURB USED. ALSO SEE PULL BOX S.D.D. 9B4

SIDE ELEVATION

DETAIL FOR CONDUIT UNDER PAVED HIGHWAYS

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

METALLIC (STANDARD SPECIFICATION 652.2.2) OR NONMETALLIC (STANDARD SPECIFICATION 652.2.3) CONDUIT SHALL BE FURNISHED AND PLACED AS SHOWN.

DEPTH OF CONDUIT INSTALLED BELOW THE TRAVELED WAY SHALL BE 24 INCHES MINIMUM AND 36 INCHES MAXIMUM.

DEPTH OF CONDUIT INSTALLED THAT IS NOT BELOW THE TRAVELED WAY SHALL BE 18 INCHES MINIMUM AND 36 INCHES MAXIMUM.

ANY EXCEPTION TO THE MAXIMUM DEPTH SHALL BE ONLY WITH THE WRITTEN APPROVAL OF THE ENGINEER.

THE TRENCH SHALL NOT BE BACKFILLED PRIOR TO INSPECTION OF THE CONDUIT.

ALL METALLIC CONDUIT RACEWAY ENDS SHALL BE REAMED AND THREADED.

ALL METALLIC CONDUIT IN WHICH WIRE OR CABLE IS TO BE INSTALLED SHALL BE BUSHED WITH APPROVED THREADED BUSHINGS BEFORE INSTALLATION OF THE WIRE OR CABLE.

ALL METALLIC CONDUITS IN WHICH WIRE OR CABLE IS NOT TO BE INSTALLED SHALL BE CAPPED WITH THREADED PROTECTIVE CAPS, AS APPROVED BY THE ENGINEER.

ALL NONMETALLIC CONDUIT SHALL BE CAPPED OR PLUGGED IMMEDIATELY AFTER INSTALLATION AND SHALL REMAIN CAPPED OR PLUGGED UNTIL WIRE/CABLES ARE INSTALLED.

NONMETALLIC CONDUITS IN WHICH WIRE OR CABLE IS NOT BEING INSTALLED SHALL REMAIN CAPPED OR PLUGGED.

BENDING OF PVC ELECTRICAL CONDUIT SHALL BE ACCOMPLISHED BY USING A BLANKET OR EMERSION TYPE TANK DESIGNED FOR THE PURPOSE OF BENDING PVC ELECTRICAL CONDUIT.

ALL CUT ENDS SHALL BE TRIMMED INSIDE AND OUTSIDE TO REMOVE ALL ROUGH EDGES ON NONMETALLIC CONDUIT. (SEE NEC 347.5)

WHEN REQUIRED TO CONNECT NONMETALLIC CONDUIT TO METALLIC CONDUIT, ONLY U.L. LISTED ADAPTER FITTINGS SHALL BE USED.

PRIOR TO CONDUIT ACCEPTANCE, CONDUIT CAPS OR PLUGS SHALL BE REMOVED, AND THE CAPS, PLUGS AND CONDUIT ENDS SHALL BE THOROUGHLY CLEANED AND THEN THE CAPS OR PLUGS REINSTALLED TO ENSURE THAT THE CAPS OR PLUGS CAN BE EASILY REMOVED IN THE FUTURE.

ALL CONDUIT BEING FURNISHED AND INSTALLED SHALL HAVE THE U.L. LABEL FIRMLY ATTACHED.

CONDUIT RUNS SHALL BE THE SAME SIZE OF CONDUIT FROM ONE END TO THE OTHER (FROM PULL BOX TO PULL BOX-OR-JUNCTION BOX TO JUNCTION BOX-OR-BASE TO BASE, ETC.).

TRACER WIRE SHALL BE INSTALLED AS STATED IN THE STANDARD SPECIFICATION, ITEM 652.3.1.1.

ALL CONDUIT RUNS SHALL BE STRAIGHT (WITHOUT BENDS) FROM PULL BOX TO PULL BOX, PULL BOX TO BASE AND BASE TO BASE AS SHOWN ON THE PLANS.

CONDUIT

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
March, 2017 — DATE
/S/ Ahmet Demirbilek — STATE ELECTRICAL
FHWA

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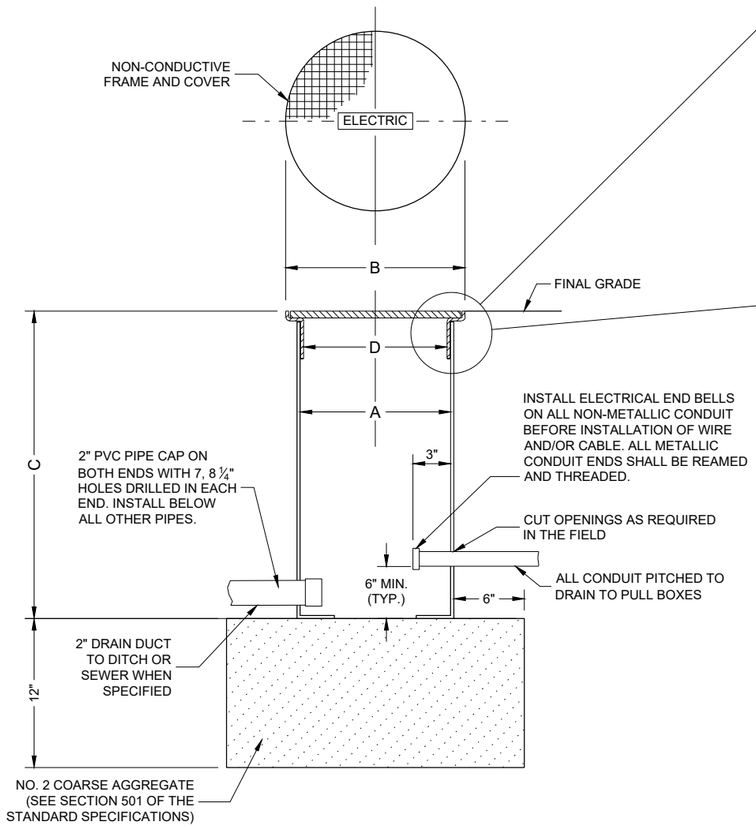
S.D.D. 9 B 2-10

TABLE OF NOMINAL DIMENSIONS AND WEIGHTS

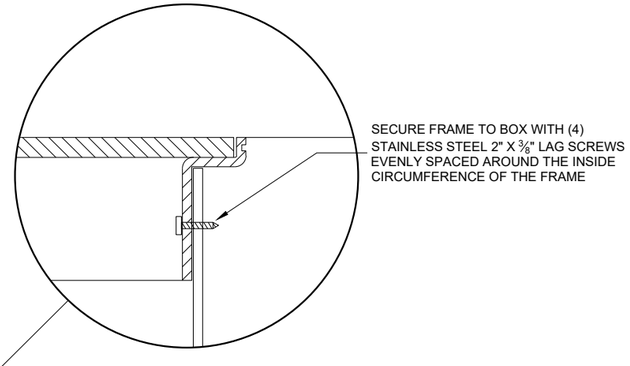
DIMENSION IN INCHES		NON- CONDUCTIVE PULL BOX	
BOX DIAMETER ** (INSIDE)	A	24	24
BOX OVERALL OUTSIDE DIAMETER	B	27	27
BOX LENGTH	C	36	42
FRAME OPENING	D	22 ½	22 ½
WEIGHT IN POUNDS *			
COVER		50	50
BOX ONLY		75	85

* THE ACTUAL WEIGHT OF THE COVER OR BOX ONLY MAY VARY NOT TO EXCEED 100 LBS INDIVIDUALLY.

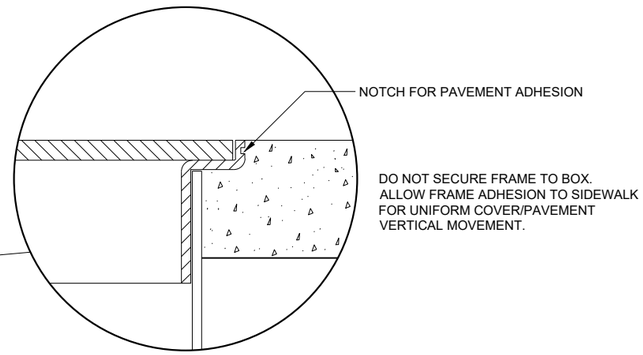
** DIAMETER VARIES FROM TOP TO BOTTOM WITH THE DIAMETER LARGER AT THE BOTTOM TO PREVENT FROST HEAVE.



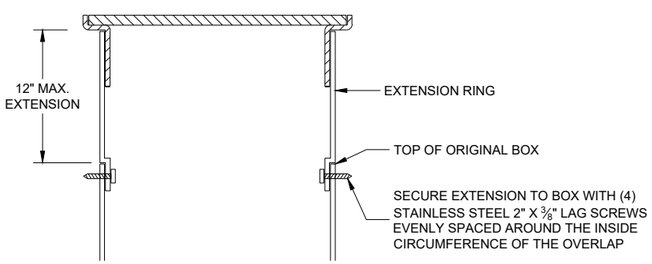
NON-CONDUCTIVE PULL BOX



INSTALLED IN SOD OR CRUSHED AGGREGATE



INSTALLED IN SIDEWALK



BOX EXTENSION

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

ALL BOXES, FRAMES AND COVERS SHALL BE SUITABLE FOR TIER 15 LOADING AS SPECIFIED IN ANSI/SCTE 77.

PROVIDE AN OPENING FOR TOOL ASSISTED COVER REMOVAL NOT LARGE ENOUGH TO PERMIT PASSAGE OF A SPHERE MORE THAN ½" DIAMETER

ENSURE COVER SURFACE IS SKID RESISTANT WITH A COEFFICIENT OF FRICTION OF AT LEAST 0.5 AND VERTICAL SURFACE DICONTINUITIES LESS THAN ¼".

COVER SHALL BE MAGNETICALLY LOCATABLE.

BOXES AND EXTENSIONS ARE TRIMMABLE FOR CUSTOM LENGTHS. TRIMMED PIECES SHALL MAINTAIN A UNIFORM LENGTH.

ENTRANCE HOLES INTO PULL BOXES SHALL BE CUT WITH A CIRCULAR HOLE SAW OR HYDRAULIC CONDUIT PUNCH. HOLE SIZE SHALL BE THE OUTSIDE DIAMETER OF THE CONDUIT THAT IS TO FIT IN THE OPENING PLUS NO MORE THAN ¼".

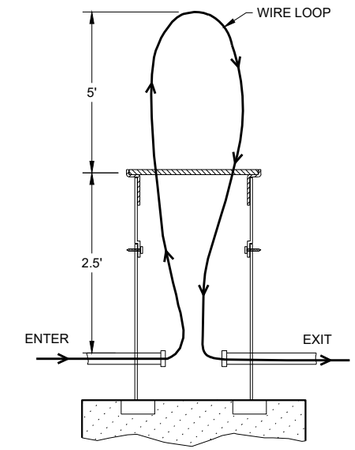
THE CONTRACTOR SHALL NOT INSTALL WIRE IN ANY PULL BOX UNTIL ITS INSTALLATION HAS BEEN INSPECTED AND ACCEPTED BY THE ENGINEER.

ALL METALLIC CONDUIT IN WHICH WIRE AND/OR CABLE IS TO BE INSTALLED, SHALL BE BUSHED BEFORE INSTALLATION OF THE WIRE AND/OR CABLE.

ENTIRE BOX MUST BE CONSTRUCTED OF NON-CONDUCTIVE MATERIALS WITH THE EXCEPTION OF STAINLESS STEEL FASTENERS AND MAGNETIC LOCATABLE DEVICE.

WHEN A PULL BOX IS INSTALLED IN CRUSHED AGGREGATE SHOULDERS, PLACE IT 2-3 INCHES BELOW GRADE AND COVER IT WITH 2-3 INCHES OF CRUSHED AGGREGATE.

LABEL ON COVER SHALL READ "ELECTRIC" FOR SIGNAL AND LIGHTING SYSTEMS, "WISDOT ITS" FOR COMMUNICATIONS AND ITS EQUIPMENT SYSTEMS.



MEASUREMENT DETAIL FOR WIRE/CABLE IN THE PULL BOX

PULL BOXES NON-CONDUCTIVE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2022
DATE
/S/ Ahmet Demirbilek
STATE ELECTRICAL ENGI

FHWA

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SDD 09B16 - 02

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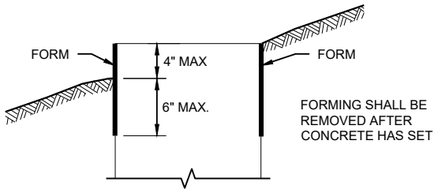
SDD 09B16 - 02

FORM DEPTH SHALL BE NO MORE THAN 6" BELOW GRADE ON THE LOWER SIDE OF BASE



FORMING DETAIL

QUANTITY REQUIREMENTS	CONCRETE BASE TYPE		
	1	2	5 & 6
APPROX. CUBIC YARDS OF CONCRETE	0.40	0.57	0.40
LBS. OF HOOP BAR STEEL	NONE	23	16
LBS. OF VERTICAL BAR STEEL	NONE	60	18

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWINGSHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

BASES SHALL BE EXCAVATED BY USE OF A CIRCULAR AUGER.

TOP SURFACES OF CONCRETE BASES SHALL BE TROWEL FINISHED SMOOTH AND LEVEL.

CONDUIT SIZES AND LOCATIONS SHALL BE SHOWN ON THE PLANS

THE FINAL OR TERMINATING CONCRETE BASE IN A CONDUIT RUN SHALL HAVE A 6" EXIT STUB INSTALLED FOR FUTURE CABLING USE. THE EXIT STUB SHALL BE SIZED AS USED THROUGHOUT THE CONDUIT RUN AS SHOWN A THE ENTRANCE OF THE BASE.

ENDS OF CONDUIT INSTALLED BELOW GRADE FRO FUTURE USE SHALL BE CAPPED IF METALLIC OR PLUGGED IF NON-METALLIC.

MINIMUM BENDING RADIUS OF CONDUIT IS EQUAL TO 6X THE DIAMETER.

CONDUIT HEIGHT ABOVE CONCRETE BASES SHALL BE 1 INCH. ALL METALLIC CONDUIT ENDS SHALL BE REAMED AND THREADED.

ALL CONDUIT ENDS AT THE TOP OF CONCRETE BASES SHALL BE CAPPED IF METALLIC OR PLUGGED IF NON-METALLIC IMMEDIATELY AFTER PLACEMENT AND BEFORE CONCRETE IS POURED. CONDUITS IN WHICH WIRE OR CABLE IS NOT INSTALLED SHALL REMAIN CAPPED OR PLUGGED.

BELL ENDS SHALL BE INSTALLED ON ALL PVC CONDUIT EXPOSED AT THE TOP OF CONCRETE BASES BEFORE INSTALLATION.

WHEN REQUIRED TO CONNECT NON-METALLIC CONDUIT TO METALLIC CONDUIT, ONLY ADAPTER FITTINGS, U.L. LISTED FOR ELECTRICAL USE, SHALL BE USED.

IF A BASE REQUIRES A DEEP FORM BECAUSE OF LOOSE DIRT OR FILL, THE FORM SHALL BE REMOVED BEFORE BACKFILLING AROUND THE BASE. BACKFILL SHALL BE TAMPED TIGHT AGAINST THE BARE CONCRETE BASE IN LAYERS OF 1 FOOT OR LESS.

A NO. 4 AWG STRANDED COPPER EQUIPMENT GROUNDING CONDUCTOR SHALL BE EXOTHERMICALLY WELDED TO THE EQUIPMENT GROUNDING ELECTRODE (GROUND ROD) FOR TYPE 2, TYPE 5 AND TYPE 6 BASES.

THE EQUIPMENT GROUNDING CONDUCTOR SHALL BE FURNISHED AND INSTALLED TO ENTER ALL BASE TYPES THROUGH A 1 INCH CONDUIT INSTALLED FOR GROUNDING PURPOSES, LEAVING A 4 FOOT COIL OF WIRE ABOVE THE CONCRETE BASE. THE EQUIPMENT GROUNDING CONDUCTOR SHALL BE NEATLY COILED AND THE COILS TIED TOGETHER.

ANCHOR RODS SHALL BE THREADED 12" IN LENGTH ON EACH END OF THE ROD. ANCHOR RODS SHALL BE MANUFACTURED IN ACCORDANCE WITH SECTION 654.2.1 OF THE STANDARD SPECIFICATIONS.

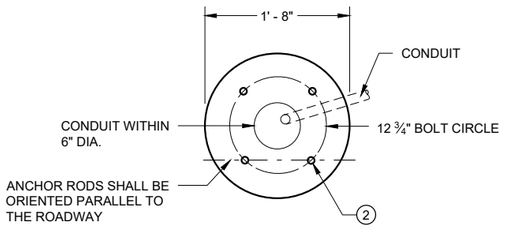
WASHERS AND LOCK WASHERS ARE REQUIRED ON ALL ANCHOR RODS.

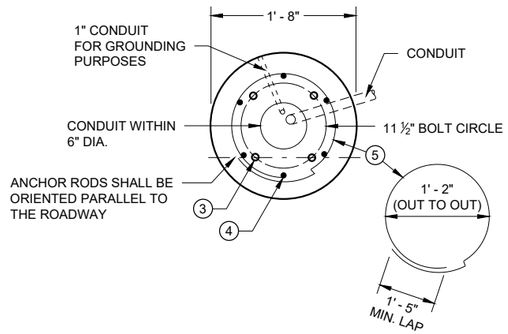
WHEN ANCHOR RODS USING THE ALTERNATE "L" BEND ARE FURNISHED, THE 4 INCH"L" BEND SHALL BE IN ADDITION TO THE SPECIFIED ANCHOR ROD BAR LENGTH. THE "L" BEND SHALL NOT BE THREADED.

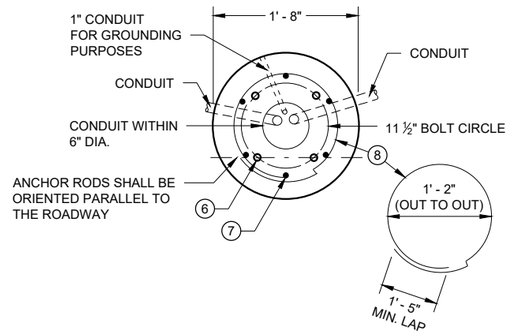
ANCHOR RODS SHALL BE INSTALLED WITH MISALIGNMENTS OF LESS THAN 1:40 FROM VERTICAL.

WELDING OF THE ANCHOR RODS TO THE CAGE IS UNACCEPTABLE. TIE WIRES SHALL BE USED.

BAR STEEL REINFORCEMENT SHALL BE COATED WITH POWDERED EPOXY RESIN IN ACCORDANCE WITH SECTION 505 OF THE STANDARD SPECIFICATIONS (LATEST EDITION).







1 THE MINIMUM DEPTH OF CONDUIT EXITING THE CONCRETE BASE AND INSTALLED BELOW THE TRAVELED WAY SHALL BE 24 INCHES. THE MINIMUM DEPTH OF CONDUIT EXITING THE CONCRETE BASE THAT IS NOT INSTALLED BELOW THE TRAVELED WAY SHALL BE 18 INCHES. THE MAXIMUM DEPTH OF ALL CONDUIT SHALL BE 36 INCHES EXCEPT WITH WRITTEN APPROVAL OF THE ENGINEER.

2 (4) 1" DIA. X 3' - 6" ANCHOR RODS.

3 (4) 1" DIA. X 5' - 0" ANCHOR RODS.

4 (6) NO. 6 X 6' - 8" BAR STEEL REINFORCEMENT.

5 (7) NO. 4 X 5' - 1" BAR STEEL REINFORCEMENT @ 1' - 0" C - C.

6 (4) 1" DIA. X 3' - 6" ANCHOR RODS.

7 (6) NO. 4 X 4' - 8" BAR STEEL REINFORCEMENT.

8 (5) NO. 4 X 5' - 1" BAR STELL REINFORCEMENT @ 1' - 0" C -C.

9 EXOTHERMIC CONNECTION TO EUIPMENT GROUNDING CONDUCTOR

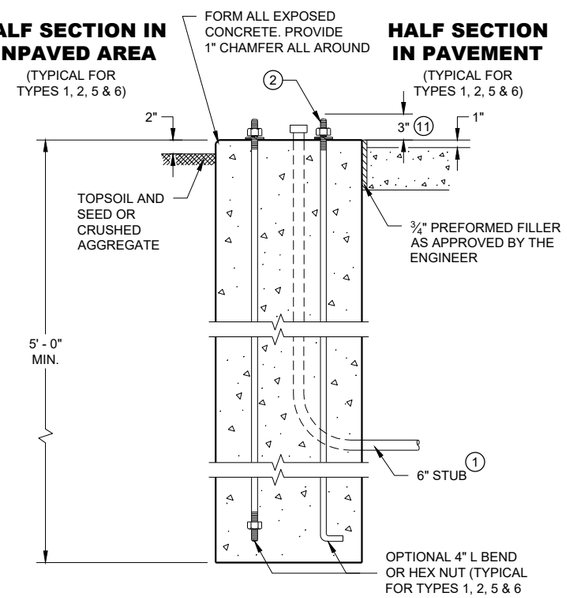
10 ⅝" DIA. X 8' -0" COPPERCLAD EQUIPMENT GROUNDING ELECTRODE REQUIRED

11 ANY ANCHOR ROD PROJECTION SHORTER THAN 2 ¾" OR LONGER THAN 3 ¼" SHALL REQUIRE THE BASE TO BE REMOVED AND REPLACED AT THE CONTRACTORS EXPENSE.

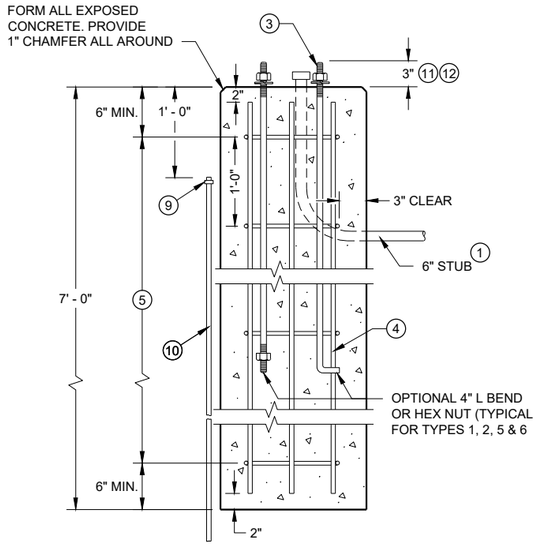
12 FOR NON - BREAKAWAY INSTALLATIONS, 4 ½" ± ANCHOR ROD PROJECTION WITH THE USE OF LEVELING NUTS. RODENT SCREEN REQUIRED.

HALF SECTION IN UNPAVED AREA
(TYPICAL FOR TYPES 1, 2, 5 & 6)

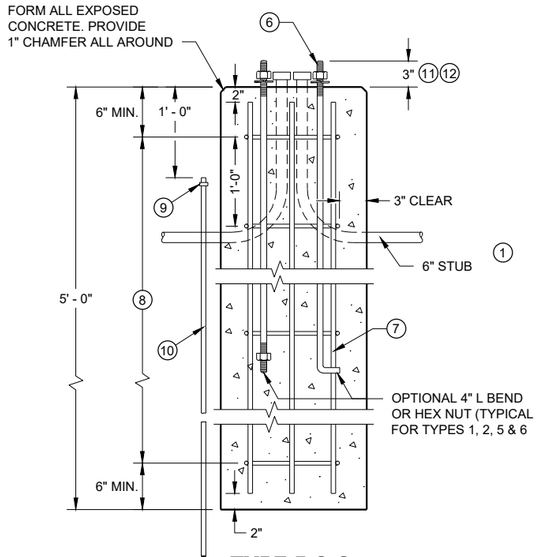
HALF SECTION IN PAVEMENT
(TYPICAL FOR TYPES 1, 2, 5 & 6)



TYPE 1



TYPE 2



TYPE 5 & 6

CONCRETE BASES

CONCRETE BASES TYPES 1, 2, 5, & 6

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2019
DATE

/S/ Ahmet Demirbilek
STATE ELECTRICAL ENGI 52

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SDD 09C02 - 09

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

FOUR (4) BOLTS SHALL BE FURNISHED WITH EACH TRANSFORMER BASE. BOLTS SHALL BE 1" DIAMETER, 4" IN LENGTH, WITH WASHERS, LOCK WASHERS AND NUTS. BOLTS, NUTS AND WASHERS SHALL BE MANUFACTURED IN ACCORDANCE WITH SECTION 531.2.2 OF THE STANDARD SPECIFICATIONS.

LEVELING SHIMS, IF NEEDED, SHALL BE DESIGNED FOR THE PURPOSE AND USED UNDER CAST BASES WHEN PLUMBING POLES OR STANDARDS DURING INSTALLATION. THE USE OF WASHERS IN LIEU OF PROPER LEVELING SHIMS IS NOT ACCEPTABLE.

SHIM LENGTH SHALL BE LONG ENOUGH TO COMPLETELY COVER THE AREA UNDER THE LENGTH AND WIDTH OF THE BASE MOUNTING FLANGE.

DOUBLE NUTTING IS NOT ACCEPTABLE FOR LEVELING OR MOUNTING PURPOSES.

A NEMA APPROVED, U.L. LISTED, COPPER WITH BRASS OR STAINLESS STEEL SET SCREW, DIRECT BURY RATED, MECHANICAL CONNECTOR (LUG), SIZED TO ACCEPT AWG. #10 TO #4 COPPER STRANDED WIRE SHALL BE FURNISHED AND INSTALLED IN THE PEDESTAL AND TRANSFORMER BASES.

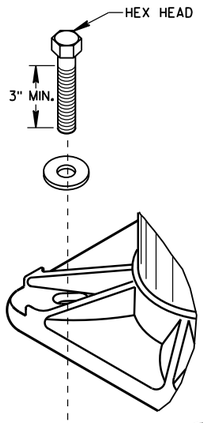
THE MECHANICAL CONNECTOR SHALL BE INSTALLED USING A 1/4" - 20 (TPI) STAINLESS STEEL HEX HEAD BOLT OF SUFFICIENT LENGTH TO FIRMLY ATTACH THE LUG TO THE BASE.

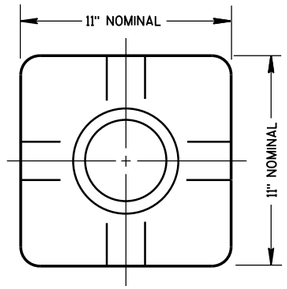
SHOULD THE MANNER OF ATTACHMENT OF THE LUG REQUIRE WASHERS, HEX NUTS, LOCK WASHER - THEY SHALL BE STAINLESS STEEL AS IS THE BOLT. THE MANNER OF ATTACHMENT SHALL NOT BLOCK ACCESSIBILITY TO WIRE PLACEMENT IN THE CONNECTOR.

PEDESTAL BASE COLLAR THREADING SHALL BE TAPERED AND IN ACCORDANCE WITH NATIONAL PIPE THREADING DIMENSIONS.

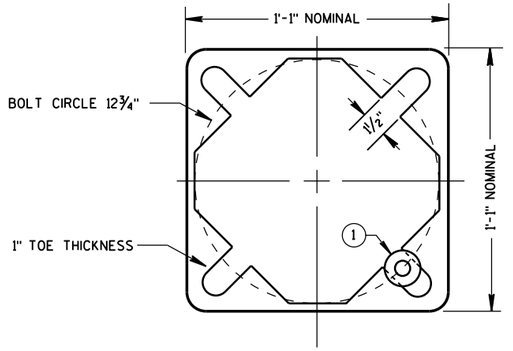
BASE COLLAR THREADING SHALL EXTEND INTO THE BASE COLLAR WITH SUFFICIENT DEPTH TO ACCEPT THE INSTALLATION OF TRAFFIC SIGNAL STANDARDS TO A DEPTH OF 1 1/2", THEN TIGHTENING TO A POINT OF BEING IMMOVABLE.

THE ACCESS DOOR SHALL BE OF THE SAME MATERIAL AS THE BASE.

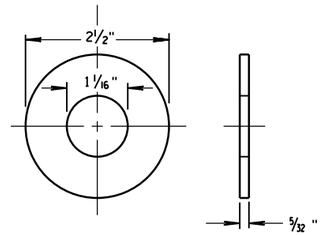




TOP VIEW
(PEDESTAL BASE)

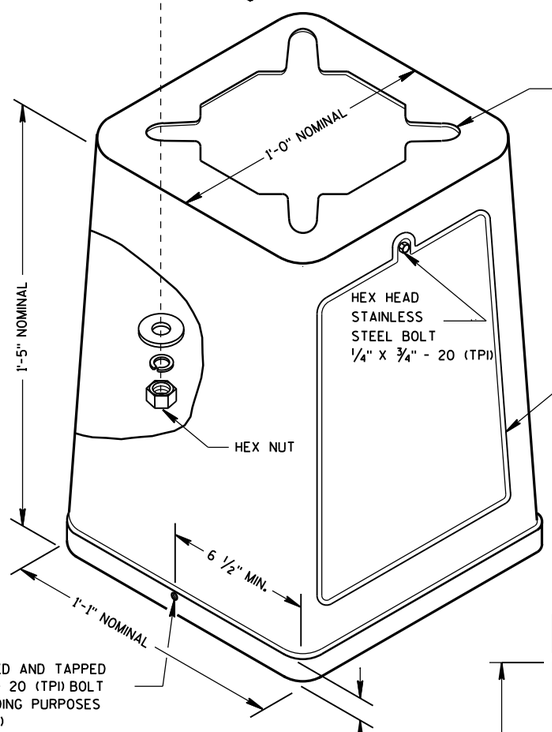


BOTTOM VIEW
(PEDESTAL BASE)

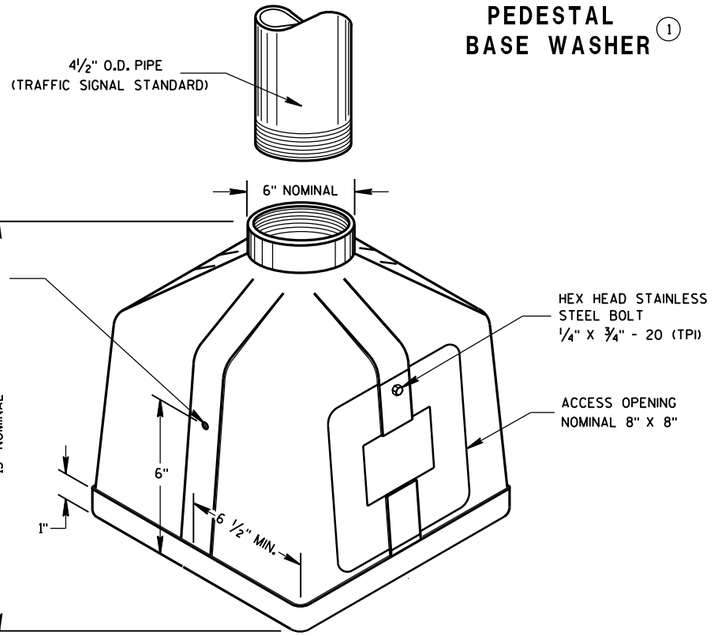


ZINC COATED STEEL WASHER
TO BE PROVIDED BY THE CONTRACTOR

PEDESTAL BASE WASHER (1)

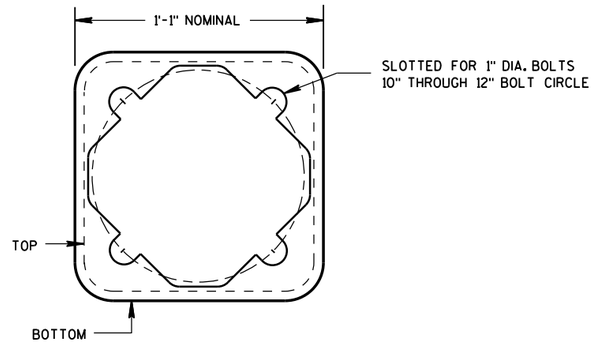


ISOMETRIC VIEW

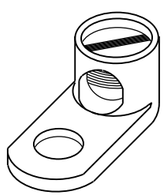


ISOMETRIC VIEW

PEDESTAL BASE



BOTTOM VIEW
(TRANSFORMER BASE)

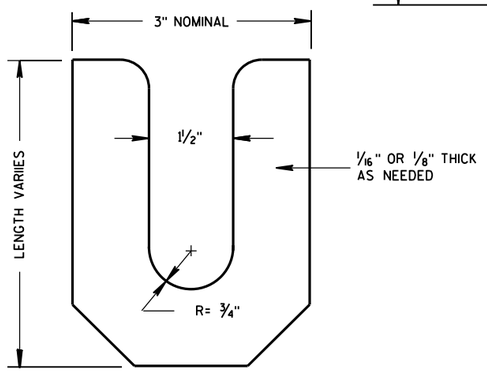


TYPICAL MECHANICAL CONNECTOR LUG

TO BE FURNISHED WITH EACH BASE

TRANSFORMER BASE

INTENDED FOR USE WITH TYPE 2, 3, 4, 5 & 6 POLES



LEVELING SHIM

TRANSFORMER/PEDESTAL BASES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
Sept. 2014 — DATE
/S/ Ahmet Demirbilek — STATE ELECTRICAL
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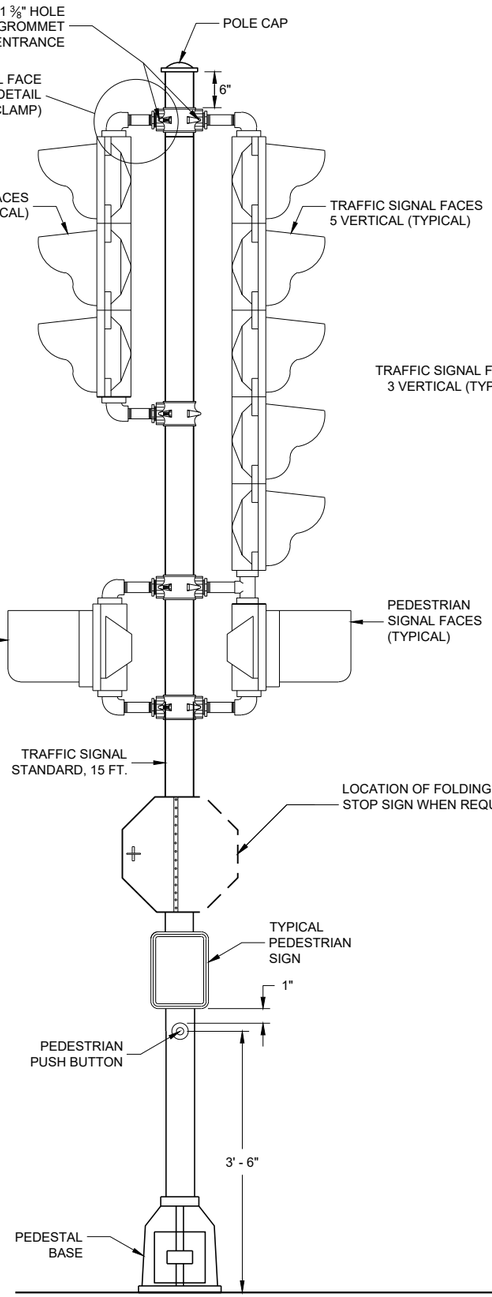
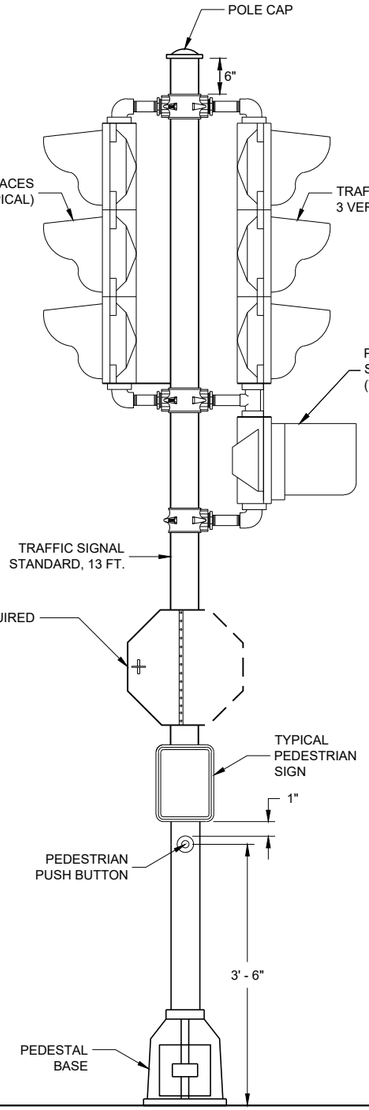
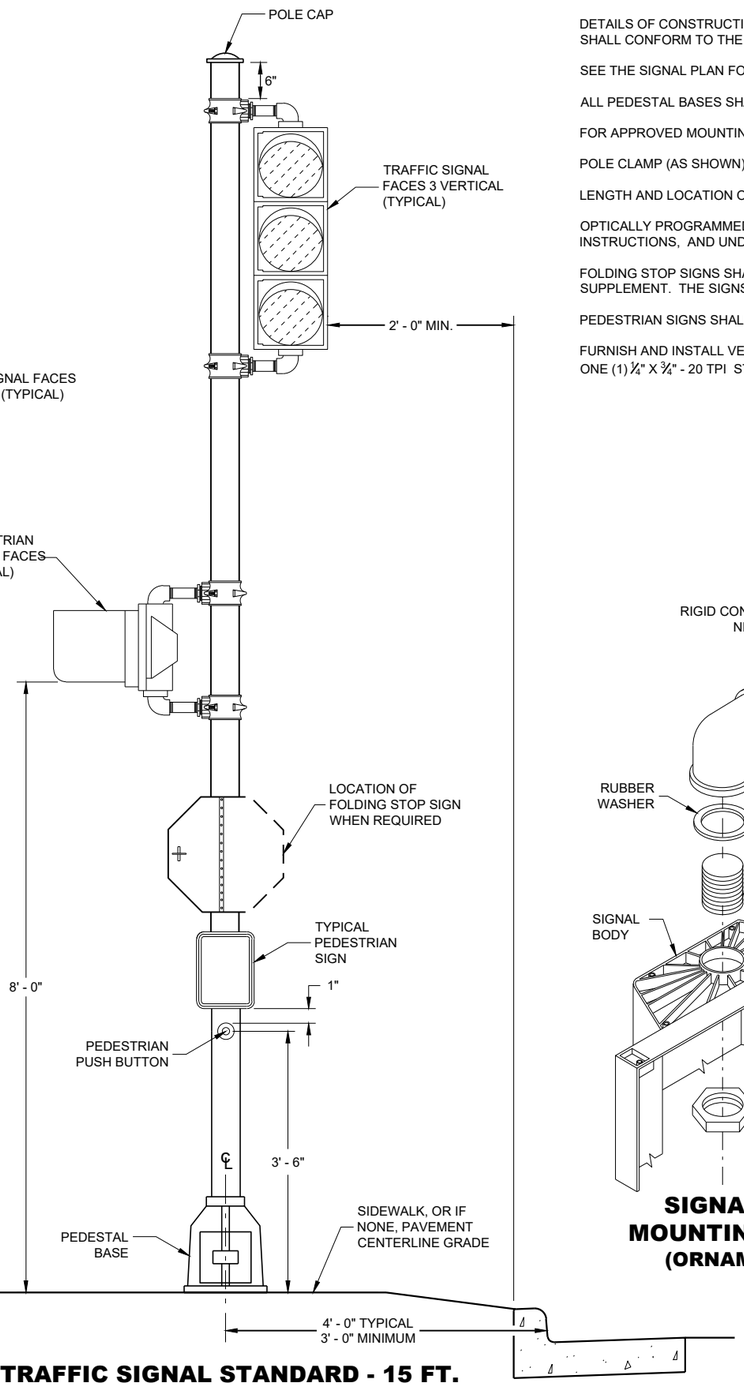
S.D.D. 9 C 3-4



TRAFFIC SIGNAL STANDARD - 15 FT.



TRAFFIC SIGNAL STANDARD - 13 FT.



TRAFFIC SIGNAL STANDARD - 15 FT.
3M MOUNTING (TYPICAL)

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

SEE THE SIGNAL PLAN FOR REQUIRED SIGNAL FACE SIZES.

ALL PEDESTAL BASES SHALL BE MOUNTED ON CONCRETE BASE - TYPE 1.

FOR APPROVED MOUNTING HARDWARE, SEE THE CONTRACT SPECIAL PROVISIONS.

POLE CLAMP (AS SHOWN) MOUNTING BRACKETS SHALL BE USED.

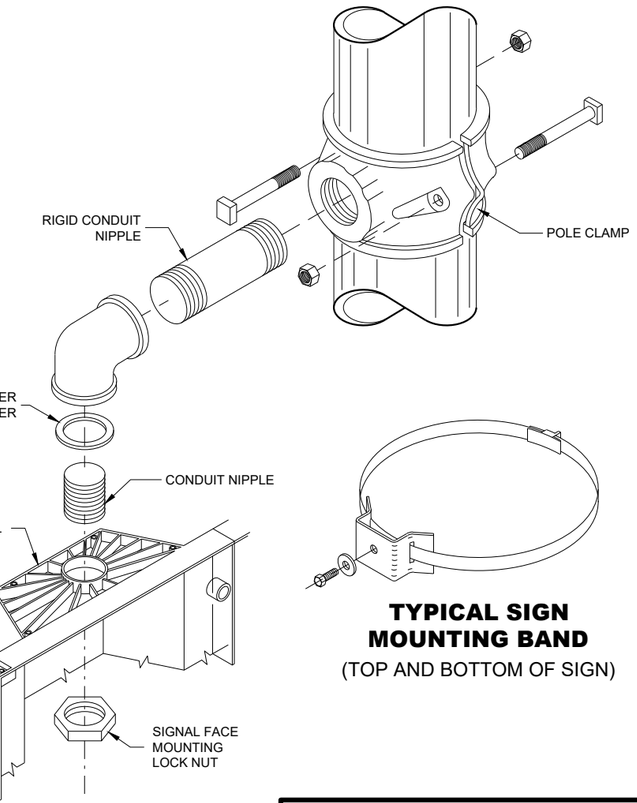
LENGTH AND LOCATION OF TRAFFIC SIGNAL STANDARDS SHALL BE AS SHOWN ON THE PLANS.

OPTICALLY PROGRAMMED SIGNAL FACES SHALL BE MASKED IN ACCORDANCE WITH MANUFACTURERS INSTRUCTIONS, AND UNDER THE DIRECTIONS OF THE REGION TRAFFIC ENGINEER.

FOLDING STOP SIGNS SHALL BE IN ACCORDANCE WITH THE MUTCD AND/OR THE LATEST WISCONSIN SUPPLEMENT. THE SIGNS SHALL BE SIZED AND LOCATED AS CALLED FOR IN IN THE PLANS.

PEDESTRIAN SIGNS SHALL BE AS DESIGNATED IN THE PLANS.

FURNISH AND INSTALL VENTILATED, CAST, METALLIC (ALUMINUM ALLOY) CAPS. FASTEN CAPS WITH ONE (1) ¼" X ¾" - 20 TPI STAINLESS STEEL, HEX HEAD BOLT.



SIGNAL FACE MOUNTING DETAIL (ORNAMENTAL)

TYPICAL SIGN MOUNTING BAND
(TOP AND BOTTOM OF SIGN)

TRAFFIC SIGNAL STANDARD ORNAMENTAL BRACKET MOUNTINGS TYPICAL FOR 13 FT. OR 15 FT.

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
2/28/2013
DATE

/S/ Ahmet Demirbilek
STATE ELECTRICAL ENGI

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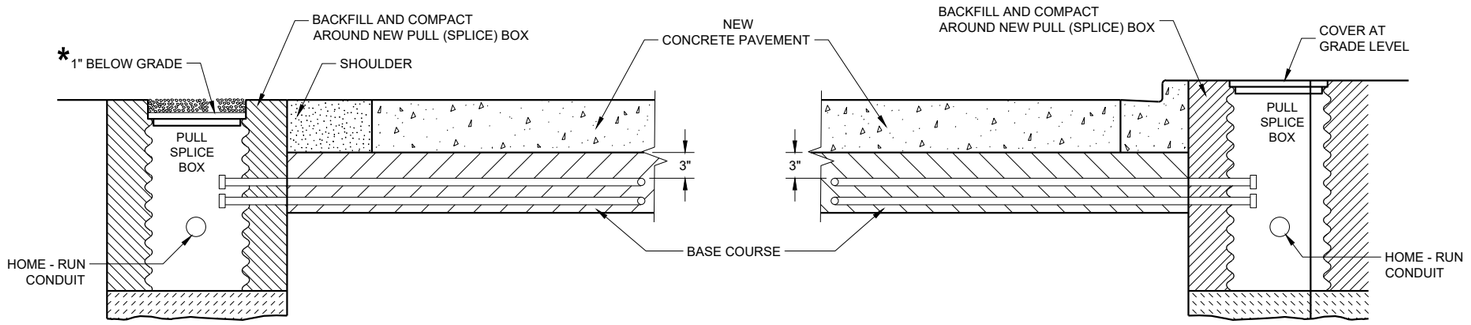


SECTION A - A
NO CURB AND GUTTER

SECTION B - B
CURB AND GUTTER

* RECESS PULL (SPLICE) BOX SO THAT THE COVER IS 3" BELOW GRADE IN SHOULDER AREAS OF CRUSHED AGGREGATE. BACKFILL OVER COVER WITH THE CRUSHED AGGREGATE TO BRING THE AREA TO GRADE LEVEL.

LOOP DETECTOR INSTALLATION DETAIL

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PITCH LEAD OUT CONDUIT TO DRAIN TO ROADSIDE PULL (SPLICE) BOX.

LOOP SIZE, LOCATION, NUMBER OF TURNS OF WIRE AND ASSOCIATED SIGNAL PHASE SHALL BE AS SHOWN ON THE PLANS.

SPLICES SHALL BE INSTALLED BY USING CAST IN PLACE SPLICE KITS LISTED ON THE DEPARTMENTS APPROVED PRODUCTS LIST OR AN ENGINEER APPROVED EQUAL. NON-INSULATED BUTT SPLICES TO FIT #12 AWG STRANDED WIRE SHALL BE USED. SPLICES SHALL BE SOLDERED AND INSULATED FROM EACH OTHER AS PER INSTRUCTIONS INCLUDED IN THE SPLICE KIT.

MEASURE GROUND RESISTANCE USING A MEGGER. REPLACE LOOP WIRE NOT ATTAINING A READING OF INFINITY TO GROUND.

AFTER SPLICING THE LOOP WIRE TO THE LOOP LEAD-IN CABLE, THE CONTRACTOR SHALL MEASURE INDUCTANCE, GROUND RESISTANCE AND WIRE RESISTANCE AT THE CABINET END OF THE LEAD-IN CABLE AND FURNISH A COPY OF THE READING TO THE PROJECT ENGINEER FOR EVALUATION.

LOOP DETECTOR LEADS SHALL BE IDENTIFIED WITH THEIR ASSOCIATED LOOP BY USE OF WATERPROOF TAGS AT BOTH ENDS OF THE CABLE. A LISTING OF THE CABLE IDENTIFICATION PER INDIVIDUAL LOOP LEAD-IN SHALL BE PLACED IN THE CABINET.

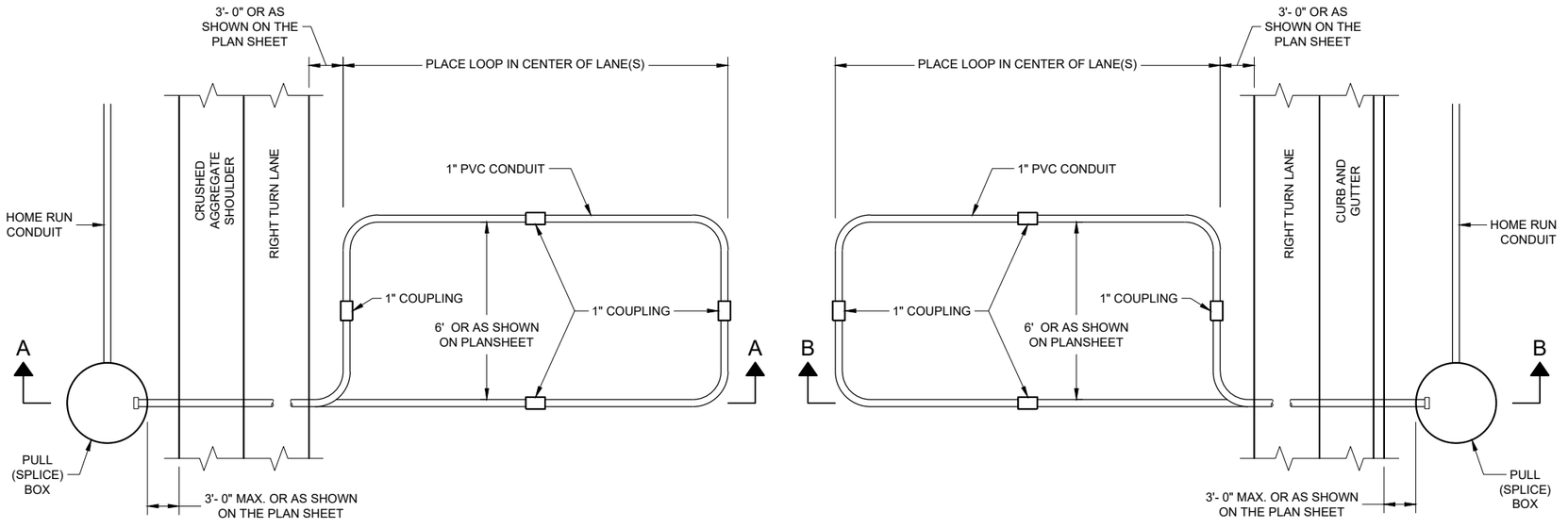
THE #12 AWG LOOP WIRE IN THE PULL (SPLICE) BOX SHALL BE HAND TWISTED AT LEAST 3 TWISTS PER FOOT BEFORE BEING SPLICED TO THE LOOP LEAD-IN CABLE.

SPLICES OF LOOP WIRE TO LEAD-IN CABLE SHALL BE MADE ONLY IN PULL (SPLICE) BOXES AT THE SIDE OF THE ROAD.

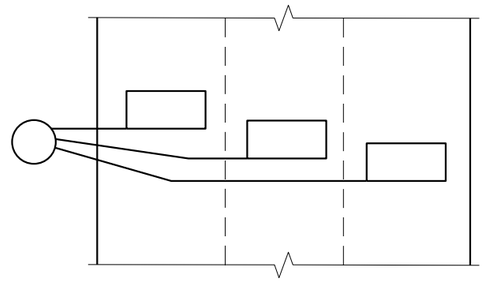
THE #12 AWG LOOP WIRE SHALL BE INSTALLED FROM THE ROADSIDE PULL (SPLICE) BOX, THROUGH THE LOOP CONDUIT, BACK TO THE ROADSIDE PULL (SPLICE) BOX, AND BE INSTALLED IN ONE NON-SPLICED, CONTINUOUS LENGTH.

PROTECTION OF THE CONDUIT IN THE BASE COURSE SHALL BE REQUIRED AFTER INSTALLATION AND BEFORE NEW PAVEMENT IS INSTALLED.

SHOULD INSTALLATION REPAIR BE REQUIRED, IT SHALL BE DONE UNDER THE DIRECTION OF THE PROJECT ENGINEER.



TYPICAL PLAN LOOP DETECTOR WITH 24" PULL (SPLICE) BOX



MULTI-LANE INSTALLATION

LOOP DETECTOR INSTALLED IN BASE COURSE WITH PULL (SPLICE) BOX OFF ROADWAY (OPTION 2)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
September 2014
DATE
/S/ Ahmet Demirbilek
STATE ELECTRICAL ENGI

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SDD 09F15 - 04b

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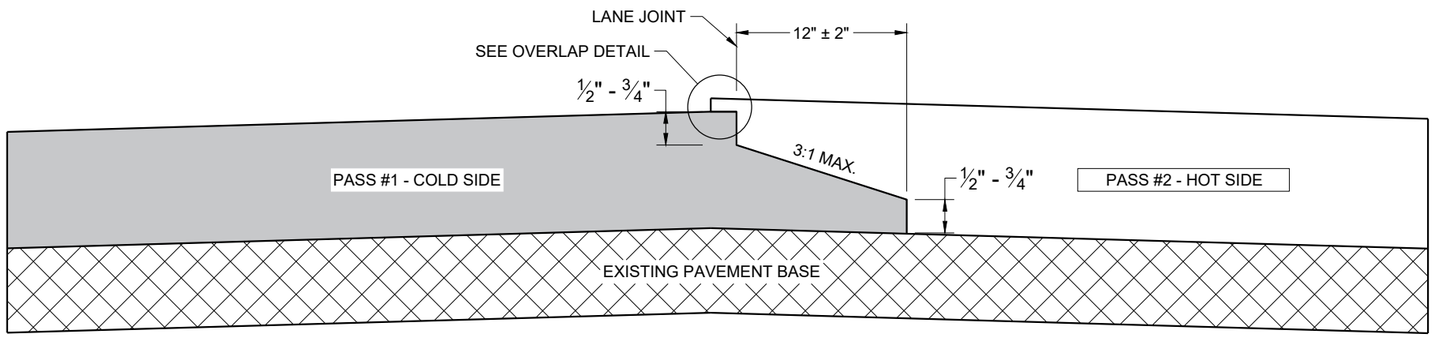
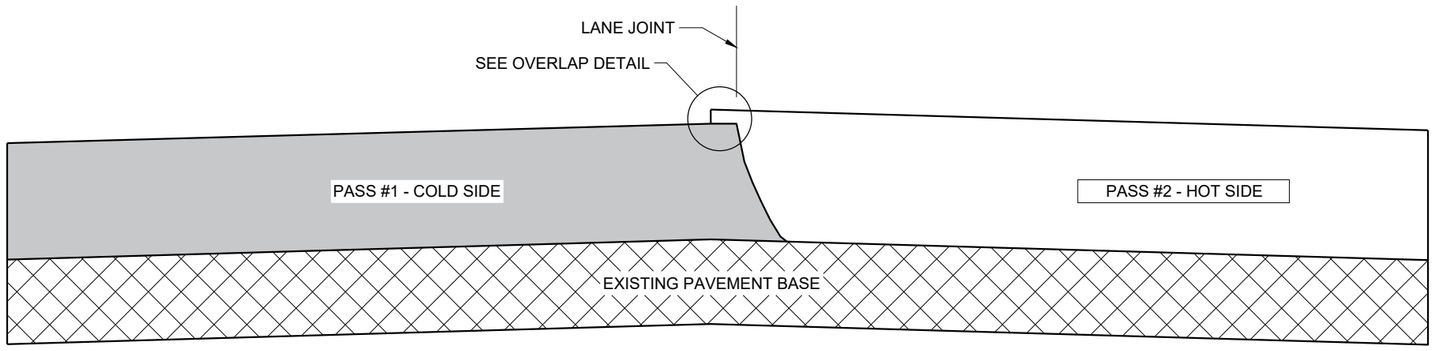
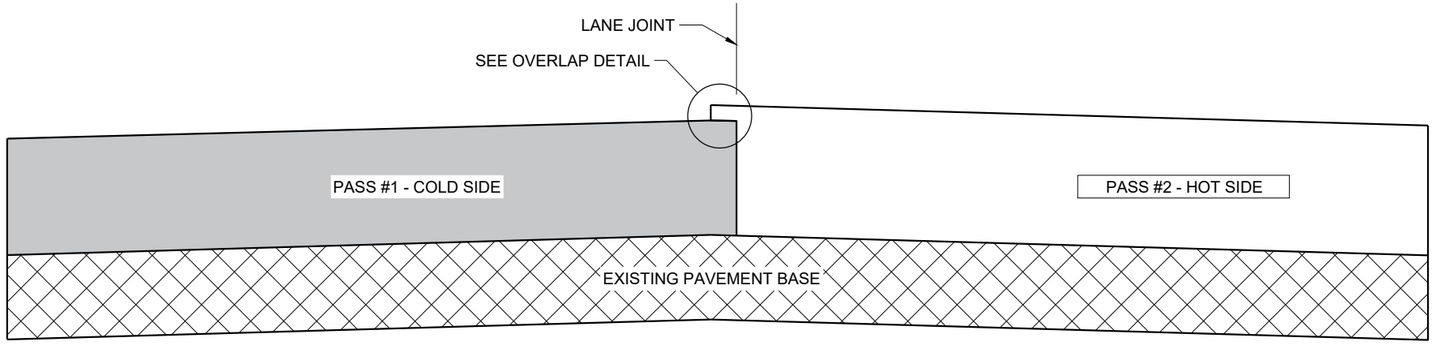


TYPICAL PAVEMENT CROSS SECTION
NOTCHED WEDGE JOINT



TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT



TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT (MILLED)

GENERAL NOTES

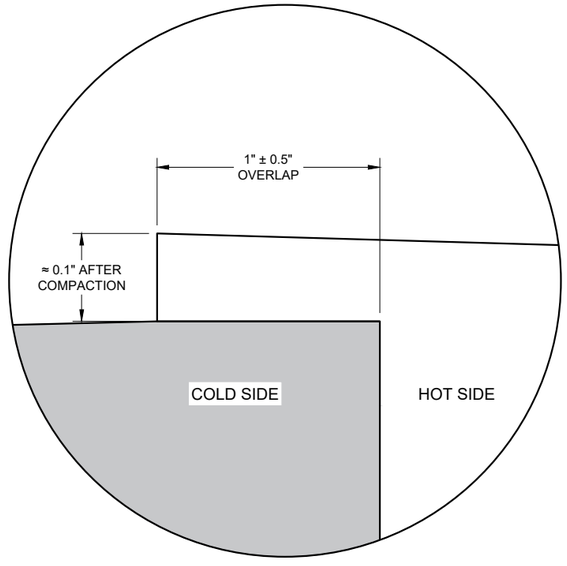
IN ADDITION TO THE DETAILS PROVIDED IN THIS DRAWING, CONFORM TO STANDARD SPECIFICATION 450.3.2.8 FOR WHEN A NOTCHED WEDGE JOINT IS REQUIRED AND FOR GENERAL JOINT CONSTRUCTION REQUIREMENTS.

FOR ALL LONGITUDINAL JOINTS, ENSURE THE PAVER SCREED OVERLAPS THE PREVIOUSLY PLACED PAVEMENT BY 1" ± 0.5" AND THE HOT SIDE OF THE JOINT REMAINS HIGHER THAN THE COLD SIDE BY APPROXIMATELY 0.1" AFTER FINAL COMPACTION. (IT WILL BE FLUSH WHEN PAVING IN ECHELON.)

ONLY REMOVE THE LONGITUDINAL NOTCHED WEDGE JOINT FOR SMA PAVEMENT OR AS DIRECTED BY THE ENGINEER TO ADDRESS SPECIFIC LENGTHS OF JOINT DAMAGED BY TRAFFIC.

WHEN MILLING BACK OR REMOVING ANY LONGITUDINAL JOINT, LIMIT THE MATERIAL REMOVED TO 2" FROM THE TOP NOTCH OR FROM THE VERTICAL JOINT EDGE ON THE COLD SIDE OF THE JOINT.

USE LONGITUDINAL MILLED JOINT AS PLANS SHOW OR THE AS THE ENGINEER DIRECTS.



OVERLAP DETAIL (TYPICAL)

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SDD 13C19 - 03

HMA LONGITUDINAL JOINTS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2020
DATE

/S/ Steven Hefel
HMA PAVEMENT ENGIN

FHWA

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

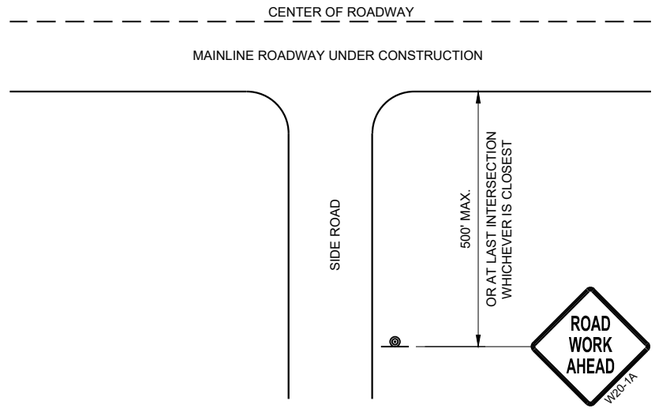
ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

* OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS

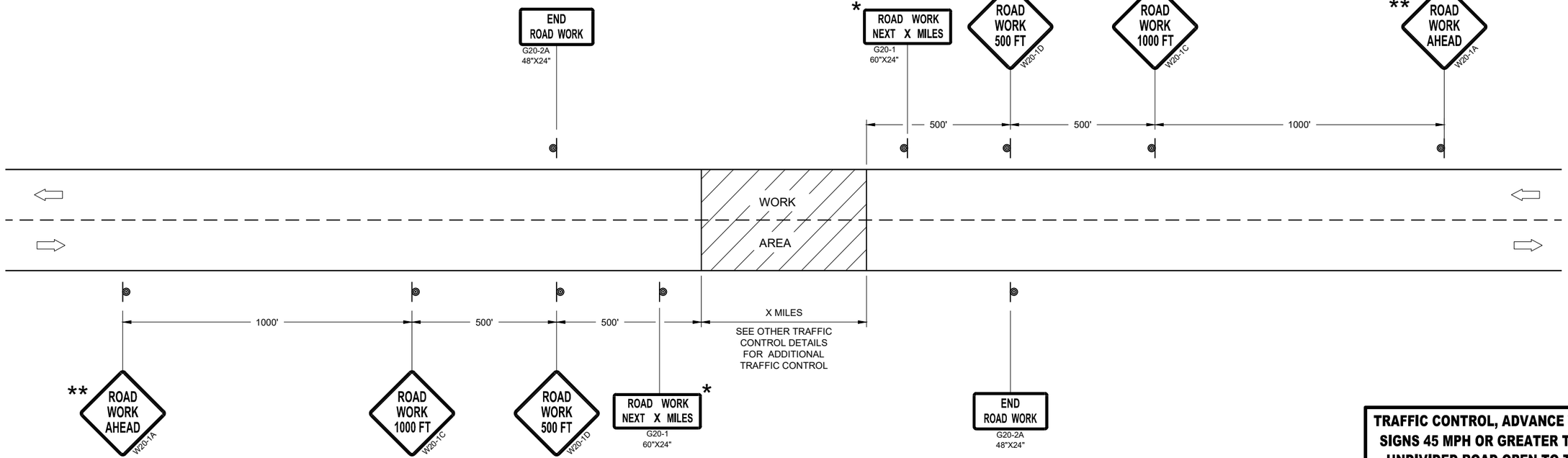
** PLACE AN ADDITIONAL W20-1A "ROAD WORK AHEAD" SIGN IF WORK AREA WITHIN THE PROJECT IS SEPARATED BY MORE THAN 2 MILES FROM PREVIOUS WORK AREA.



TYPICAL SIDE ROAD APPROACH WARNING SIGN DETAIL

LEGEND

- SIGN ON PERMANENT SUPPORT
- DIRECTION OF TRAFFIC
- WORK AREA



TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45MPH OR GREATER

TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 MPH OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
July 2018
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEE

FHWA

6

SDD 15C04 - 05

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

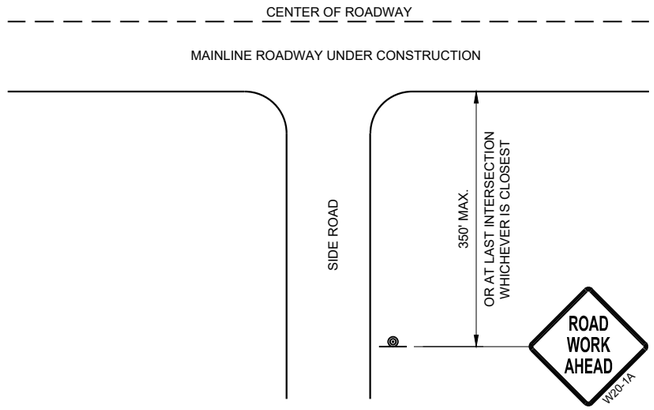
THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS, 36"X36" SIGNS MAY BE USED INSTEAD OF 48" X 48" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

* THE THIRD W20-1 SIGN IS REQUIRED ONLY IF THERE IS AN INTERSECTION BETWEEN THE "ROAD WORK 500 FEET" SIGN AND THE WORK ZONE. ADJUST THE PLACEMENT OF THIS SIGN BASED ON INTERSECTION LOCATION AND OTHER FIELD CONDITIONS.



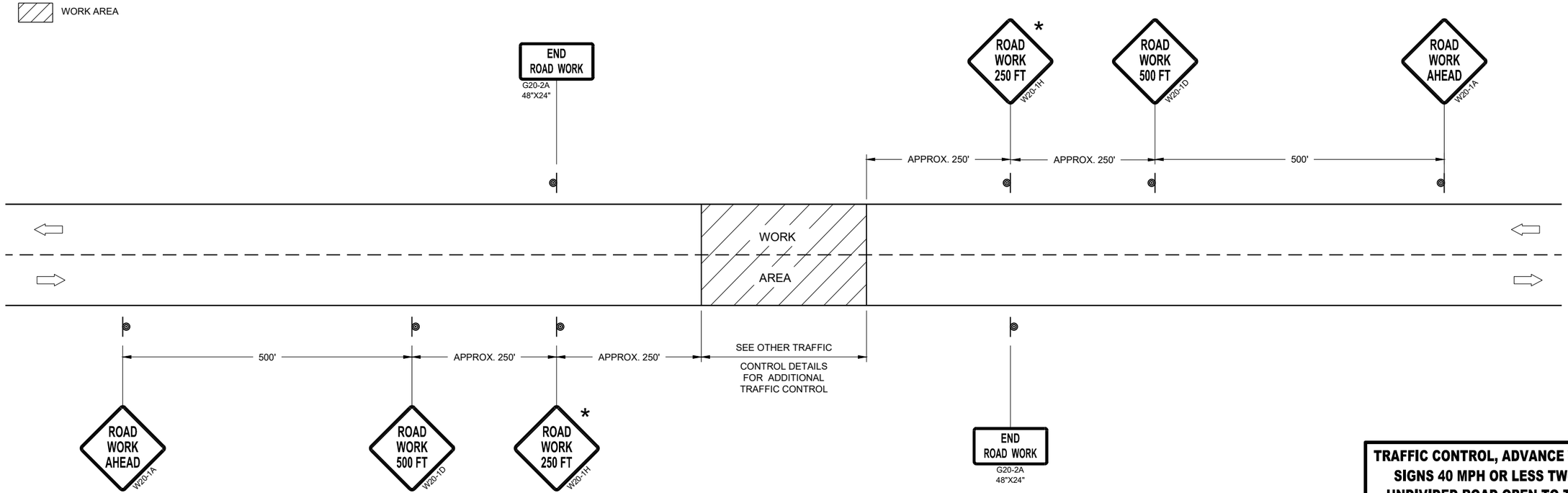
TYPICAL SIDE ROAD APPROACH WARNING SIGN DETAIL

LEGEND

SIGN ON PERMANENT SUPPORT

DIRECTION OF TRAFFIC

WORK AREA



TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40MPH OR LESS

TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 MPH OR LESS TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
July 2018
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEE

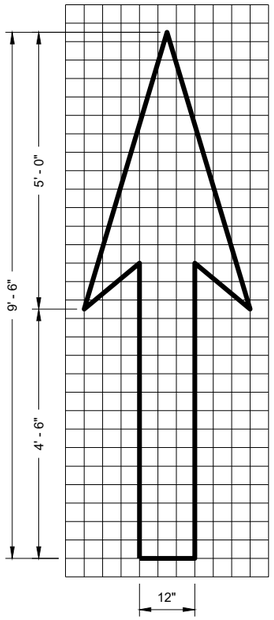
FHWA

6

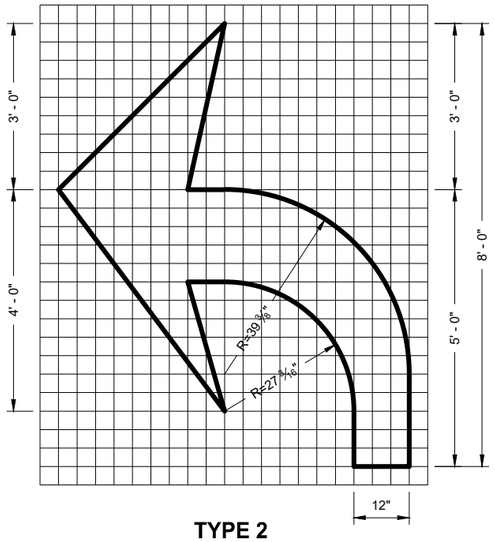
SDD 15C05 - 05

GENERAL NOTES

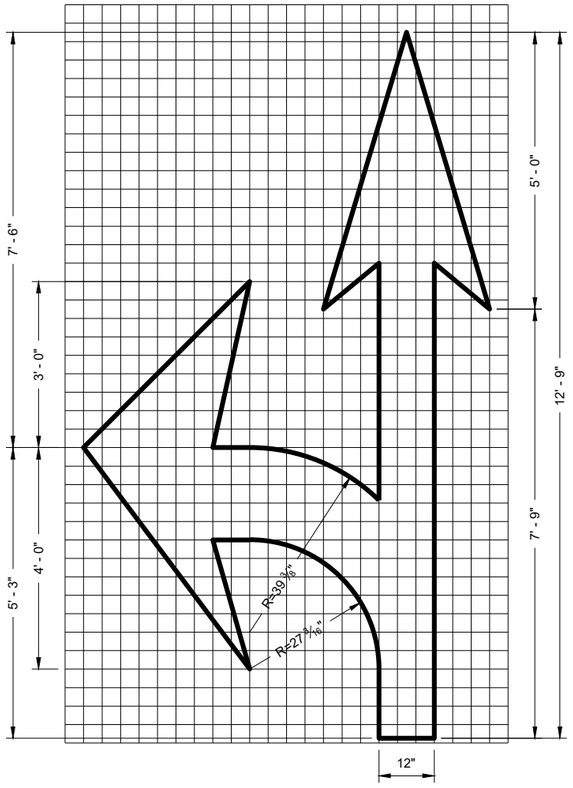
DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.



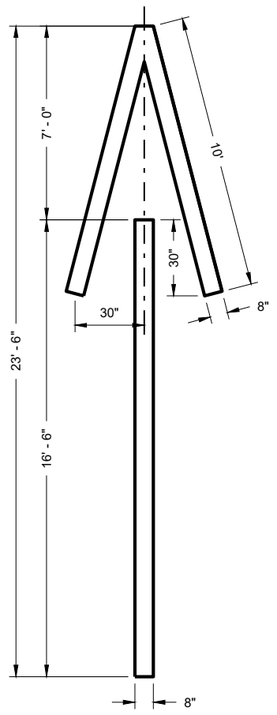
TYPE 1



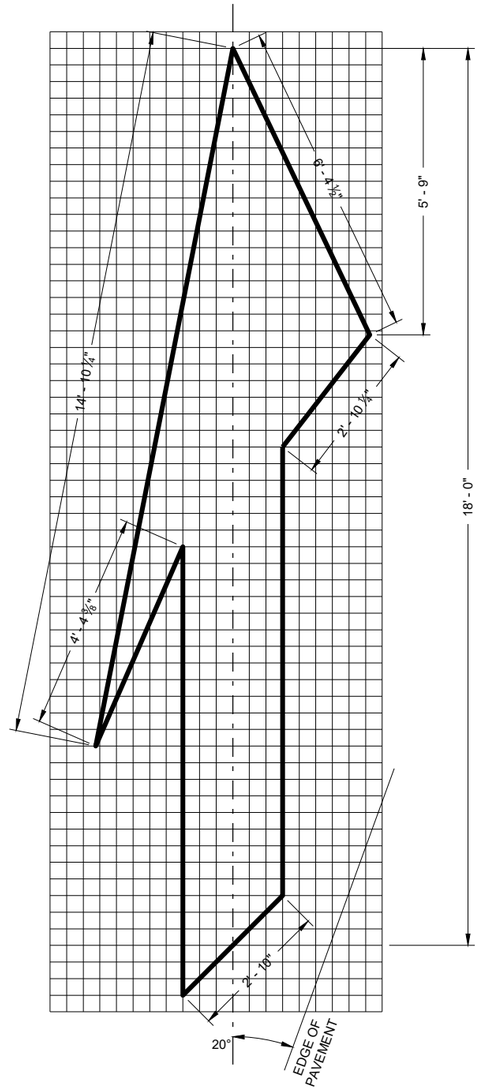
TYPE 2



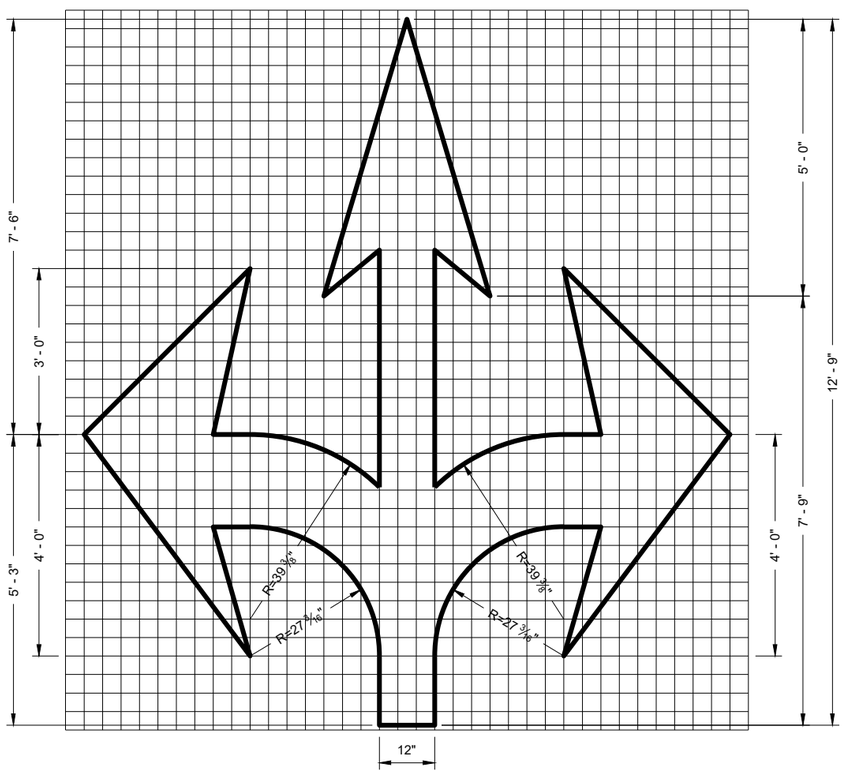
TYPE 3



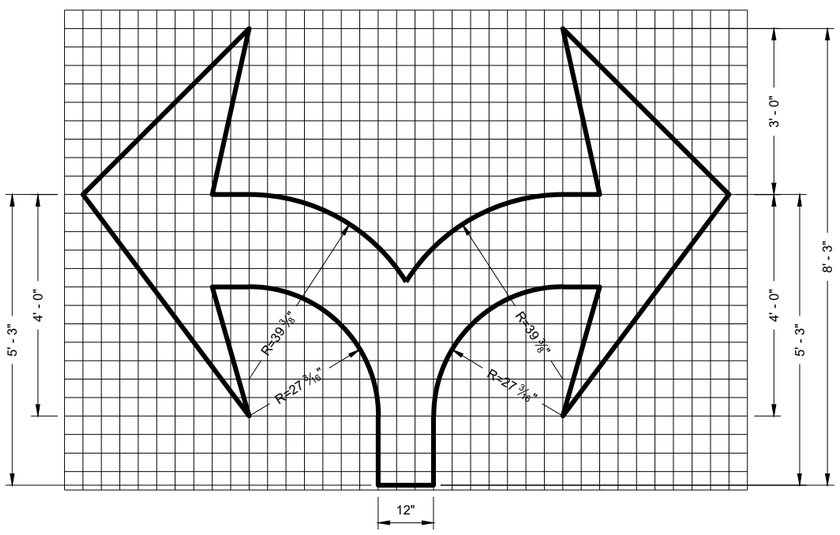
TYPE 4



TYPE 5 LANE DROP ARROW



TYPE 6



TYPE 7

PAVEMENT MARKING ARROWS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2019
DATE

/S/ Matthew Rauch
STATE SIGNING AND MAF ENGINEER

FHWA

6

SDD 15C07 - 15c

6

SDD 15C07 - 15c

GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

(1) LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING

(2) MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

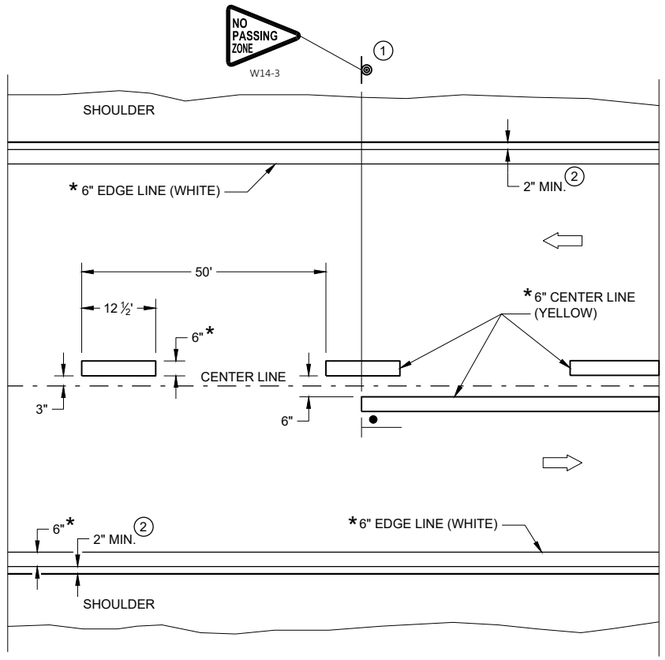
* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES

LEGEND

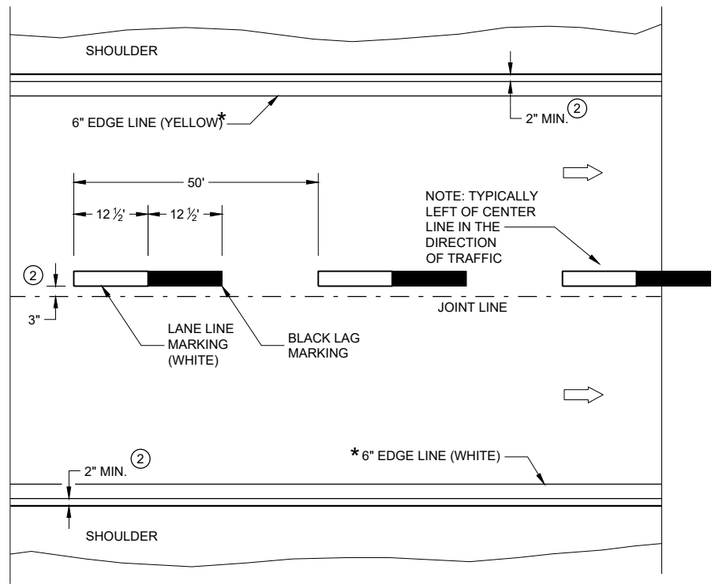
"T" MARKING

SIGN ON PERMANENT SUPPORT

DIRECTION OF TRAFFIC



TWO WAY TRAFFIC



ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING

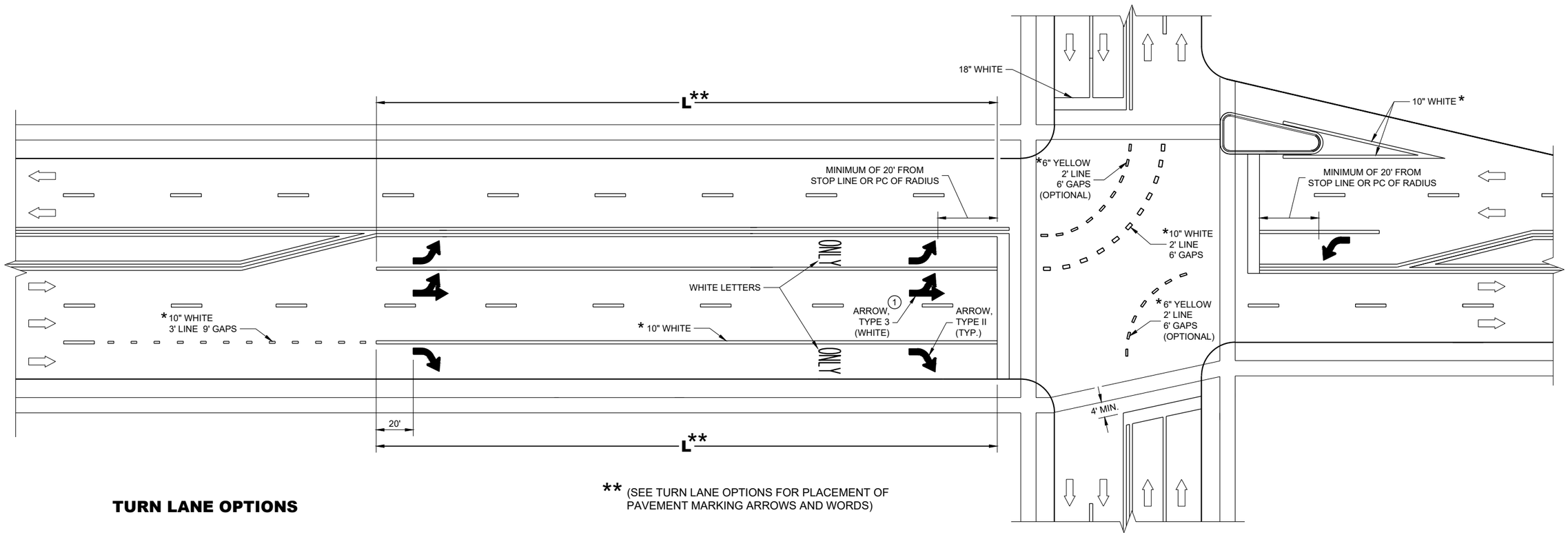
PERMANENT LONGITUDINAL PAVEMENT MARKINGS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 — DATE
/S/ Jeannie Silver — STATEWIDE SIGNING AND M ENGINEER
FHWA

6

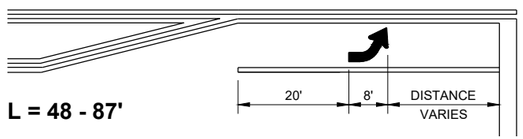
SDD 15C08-23a



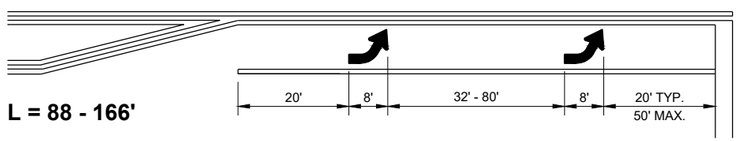
** (SEE TURN LANE OPTIONS FOR PLACEMENT OF PAVEMENT MARKING ARROWS AND WORDS)

TURN LANE OPTIONS

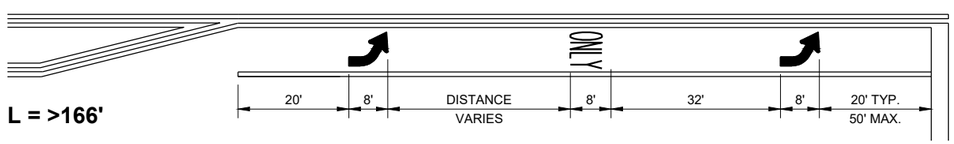
LENGTH OF TURN BAY (**L**) OF 0 - 47' DOES NOT REQUIRE PAVEMENT MARKING ARROWS OR WORDS



L = 48 - 87'



L = 88 - 166'



L = >166'

GENERAL NOTES

(1) QUANTITY AND LOCATION OF TYPE 3 ARROWS ARE THE SAME AS THE TYPE II ARROWS IN THE ADJACENT TURN LANE. FOR TURN LANES WITH A PHYSICAL SEPARATION IN THE SAME DIRECTION OF TRAVEL, THE ARROWS AND "ONLY" MARKING MAY BE ELIMINATED.

DIRECTION OF TRAFFIC

L = LENGTH OF TURN BAY

* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES

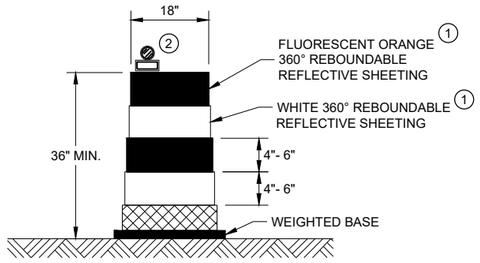
PAVEMENT MARKING (TURN LANES)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6
SDD 15C08-23d

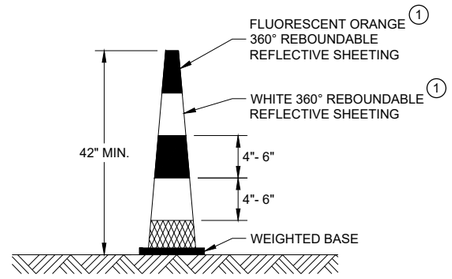
GENERAL NOTES

1. REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
2. LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.



DRUM

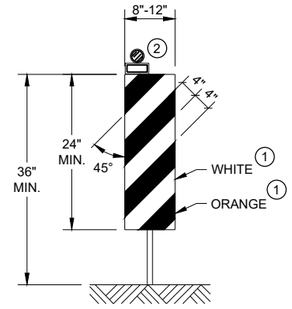
BALLAST WIDTHS
RANGE FROM 24"-36"



42" CONE

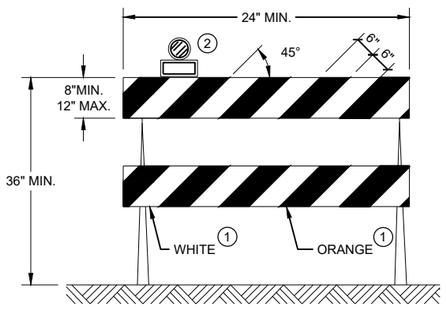
DO NOT USE IN TAPERS
½ SPACING OF DRUMS

BALLAST WIDTHS
RANGE FROM 14"-20"



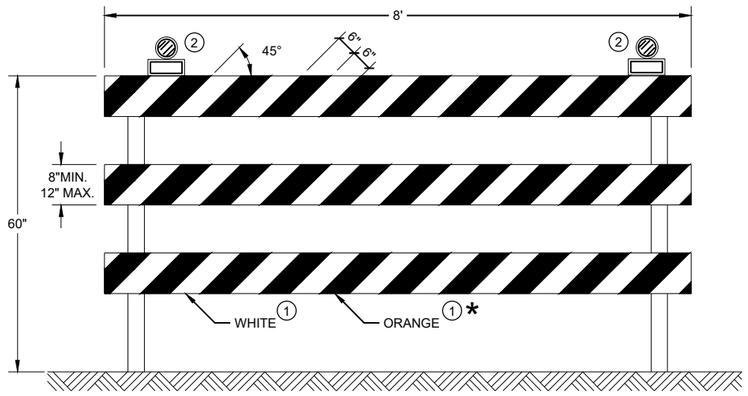
VERTICAL PANEL

THE STRIPES SHALL SLOPE DOWNWARD TO
THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE II BARRICADE

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES
MAY BE USED. ALL STRIPES SHALL SLOPE DOWNWARD
TO THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE III BARRICADE

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP
TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.

* IF USED FOR A PERMANENT APPLICATION USE RED SHEETING.

CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2022
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEE

FHWA

6

SDD 15C11 - 10b
SDD 15C11 - 10b

LEGEND

V1 MARKING VEHICLE

V2 SHADOW VEHICLE

V3 TRAIL VEHICLE

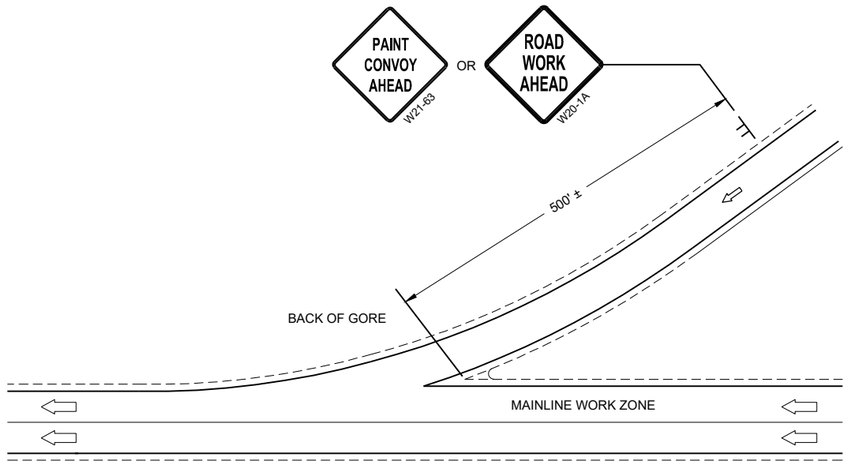
TRUCK MOUNTED ATTENUATOR (TMA)

SIGN ON TEMPORARY SUPPORT

DIRECTION OF TRAFFIC

FLASHING ARROW PANEL (MERGE)

FLASHING ARROW PANEL (CAUTION)



GENERAL NOTES

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

ALL VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.

DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL AND HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.

WHEN WORK ACTIVITY BLOCKS THE LEFT LANE, REVERSE TRAFFIC CONTROL.

WHEN A RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, PROVIDE ADDITIONAL TRAFFIC CONTROLS AS SPECIFIED IN THE CONTRACT OR AS APPROVED BY THE ENGINEER.

USE AN ATTENUATOR ON THE REARMOST VEHICLE THAT BLOCKS ALL OR PART OF THE TRAFFIC LANE.

IF THE SHOULDER IS TOO NARROW TO ACCOMMODATE THE LAST TRAILING VEHICLE, THE VEHICLE SHOULD STRADDLE THE EDGE LINE.

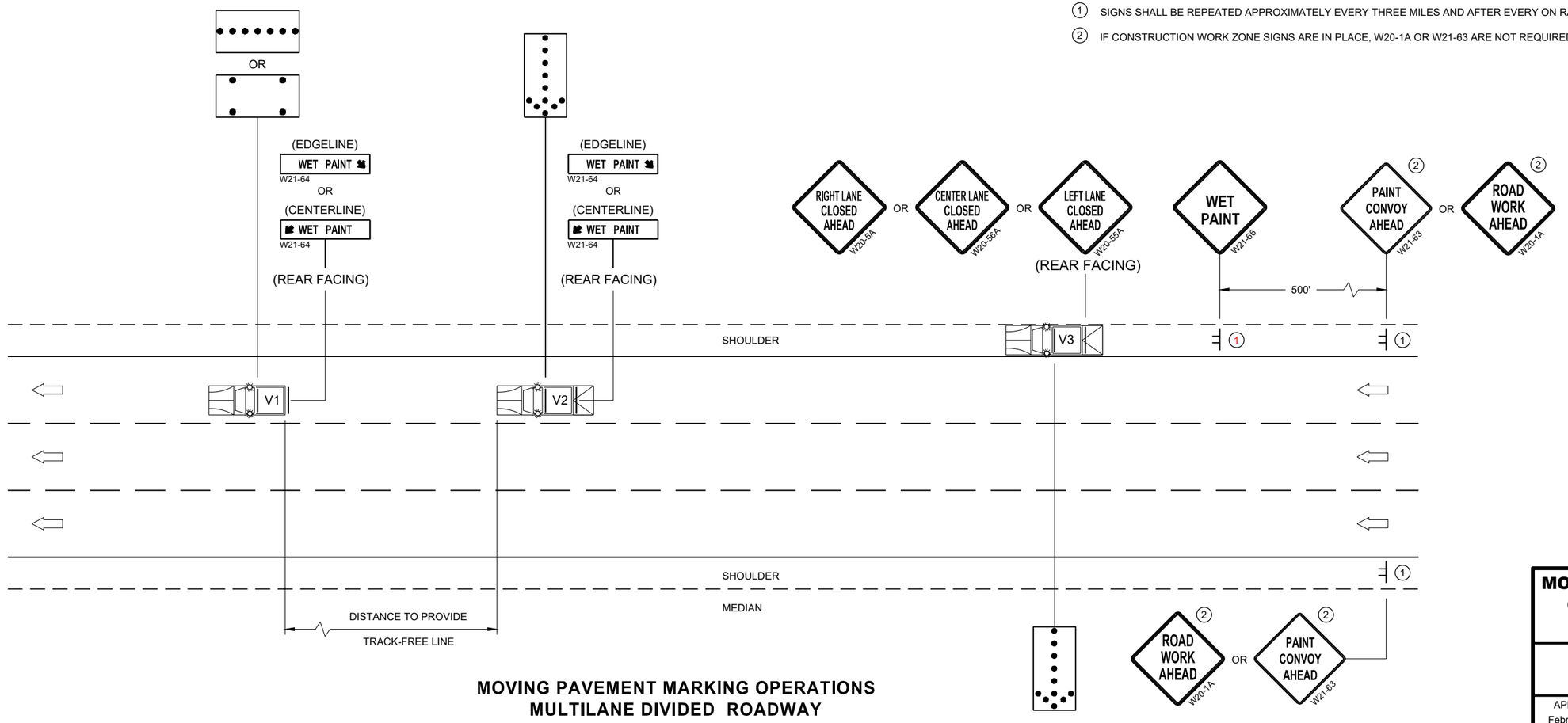
WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR LAY STATIONARY SIGNS AND SUPPORTS FLAT ON THE GRADE WITH UPRIGHTS ORIENTED PARALLEL TO AND DOWNSTREAM FROM TRAFFIC

CONES SHOULD BE USED BETWEEN THE MARKING AND SHADOW VEHICLE AT 100 FOOT SPACING. CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.

CONES SHALL BE A MINIMUM HEIGHT OF 28" FOR WET PAVEMENT MARKINGS

(1) SIGNS SHALL BE REPEATED APPROXIMATELY EVERY THREE MILES AND AFTER EVERY ON RAMP.

(2) IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1A OR W21-63 ARE NOT REQUIRED.



MOVING PAVEMENT MARKING OPERATIONS
MULTILANE DIVIDED ROADWAY

MOVING PAVEMENT MARKING OPERATION MULTI-LANE DIVIDED ROADWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2023
DATE

/S/ Andrew I...
WORK ZONE EN

FHWA

6

SDD 15C19-08c

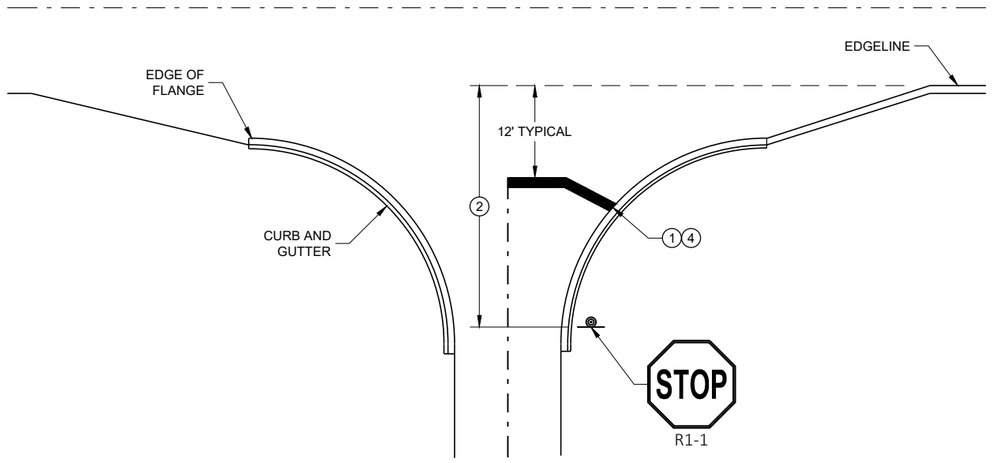
6

SDD 15C19-08c

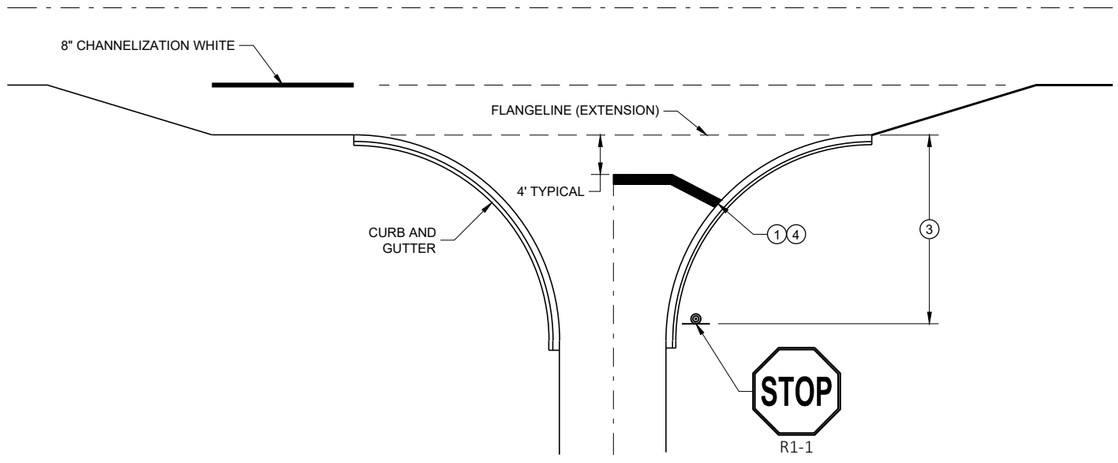
GENERAL NOTES

STOP SIGN SHALL BE PLACED A MINIMUM OF 6 FEET TO A MAXIMUM OF 50 FEET FROM THE EDGELINE LOCATION.

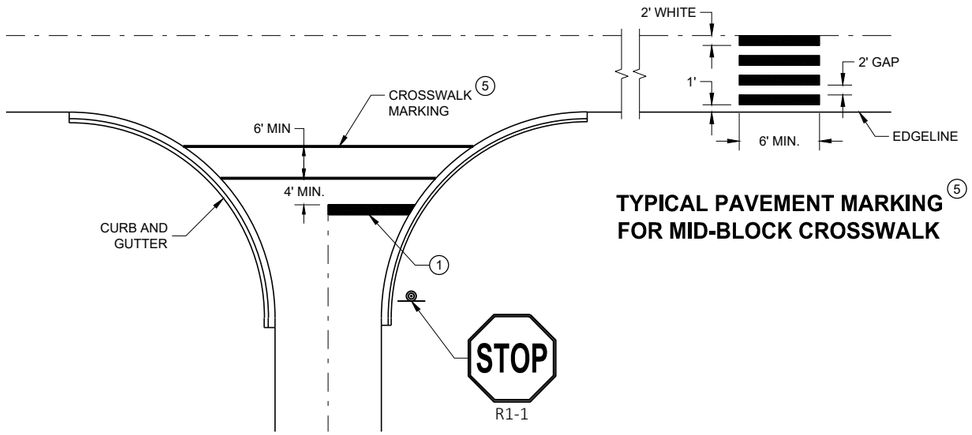
1. 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE REGION MARKING ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
2. NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGELINE.
3. NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGELINE EXTENSION.
4. MOVE CLOSER TO THE EDGE OF TRAVEL LINE AS NEEDED FOR VISIBILITY AND SIGHT LINES (NO CLOSER THAN 4 FEET).
5. LADDER BAR CROSSWALKS SHOULD ONLY BE USED FOR MID BLOCK CROSSINGS. USE 2 - 6" TRANSVERSE LINES INSTEAD.



TYPICAL STOP LINE PAVEMENT MARKING WITH CURB AND GUTTER

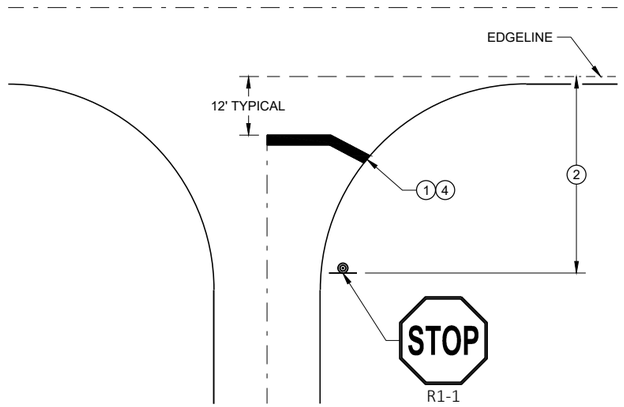


TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH RIGHT TURN LANE



TYPICAL PAVEMENT MARKING FOR MID-BLOCK CROSSWALK 5

TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH CROSSWALK MARKING



TYPICAL STOP LINE PAVEMENT MARKING WITHOUT CURB AND GUTTER

STOP LINE AND CROSSWALK PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2019 — DATE
/S/ Matthew Rauch — STATE SIGNING AND MARKING ENGINEER

FHWA

6

SDD 15C33 - 04

6

SDD 15C33 - 04

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS.

THIS LANE CLOSURE IS TYPICAL FOR CLOSING RIGHT LANE - REVERSE FOR CLOSING LEFT LANE.

ALL SIGNS ARE 48" x 48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED OR AS APPROVED BY THE ENGINEER.

FOR A LANE CLOSURE THAT IS IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, THE ADVANCED WARNING SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING LINE IF LANE CLOSURE IS TO BE IN PLACE 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

IF THE HORIZONTAL ALIGNMENT IS SUCH THAT A CURVE MAY REQUIRE ADDITIONAL DELINEATION, THE DEVICE SPACING MAY BE DECREASED TO 50 FEET.

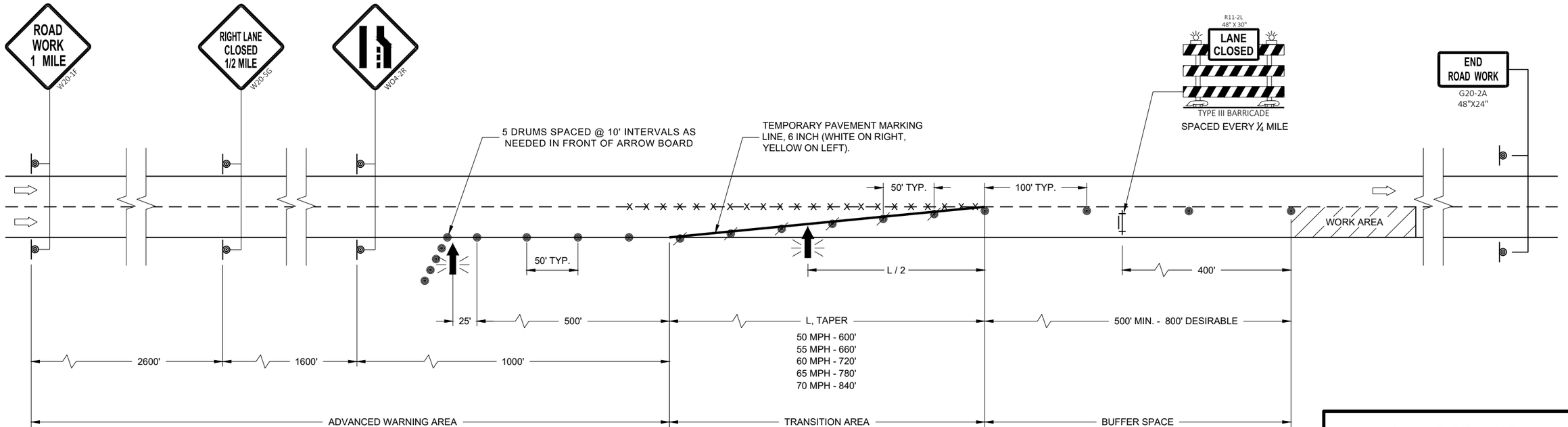
WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

ADJUSTMENTS IN BUFFER SPACE NEED TO BE INCORPORATED WHEN THE LANE CLOSURE OCCURS NEAR AN INTERCHANGE EXIT OR ENTRANCE RAMP OR INTERSECTION. THE LANE CLOSURE MUST TAKE PLACE FAR ENOUGH IN ADVANCE OF AN EXIT OR ENTRANCE RAMP TO STILL ALLOW FOR ADEQUATE BUFFER SPACE. THE MINIMUM LENGTH OF THE BUFFER SPACE BEFORE AN EXIT RAMP SHOULD BE ONE HALF THE LENGTH OF THE TRANSITION AREA. THE ENTRANCE RAMP SHOULD BE FOLLOWED BY THE ORIGINAL BUFFER SPACE LENGTH OF 800 FEET DESIRABLE PRIOR TO ANOTHER TRAFFIC CONTROL CHANGE SUCH AS A CROSSOVER MANEUVER.

CONSIDER ROADWAY GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARD SO THE DRIVER HAS A CLEAR VIEW OF THE ARROW BOARD AND LANE CLOSURE DRUMS.

LEGEND

- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- REMOVING PAVEMENT MARKINGS
- DIRECTION OF TRAFFIC
- WORK AREA
- FLASHING ARROW BOARD



TRAFFIC CONTROL LANE CLOSURE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEE

FHWA

6

SDD 15D12 - 11a

6

SDD 15D12 - 11a

LEGEND

- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- FLASHING ARROW BOARD
- DIRECTION OF TRAFFIC
- REMOVE PAVEMENT MARKING (SEE GENERAL NOTES)
- WORK AREA

GENERAL NOTES

FOR WORK ON ROADWAYS WITH SPEEDS GREATER THAN 45MPH, USE SDD 15D12.

THIS LANE CLOSURE DETAIL IS TYPICAL FOR CLOSING THE LEFT LANE. FOR A RIGHT LANE CLOSURE, REVERSE THE TRAFFIC CONTROL.

THIS DETAIL MAY BE USED FOR ROADWAYS WITH EITHER TWO OR THREE LANES IN EACH DIRECTION.

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36"X 36" SIGNS MAY BE USED IF APPROVED BY REGIONAL TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON COVERED OR "DOWNED" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS OR THAT WILL BE PLACED IN A CLOSED LANE MAY BE MOUNTED ON TEMPORARY SUPPORTS.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500' DESIRABLE) DISTANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE

W20-1A, G20-1 AND G20-2A SIGNS ARE NOT REQUIRED IF THE LANE CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING LINE IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARDS SO THE APPROACHING DRIVER HAS A CLEAR VIEW OF THE ARROW BOARDS AND LANE CLOSURE DRUMS FOR A MINIMUM 1500 FEET IN FRONT OF DRUMS.

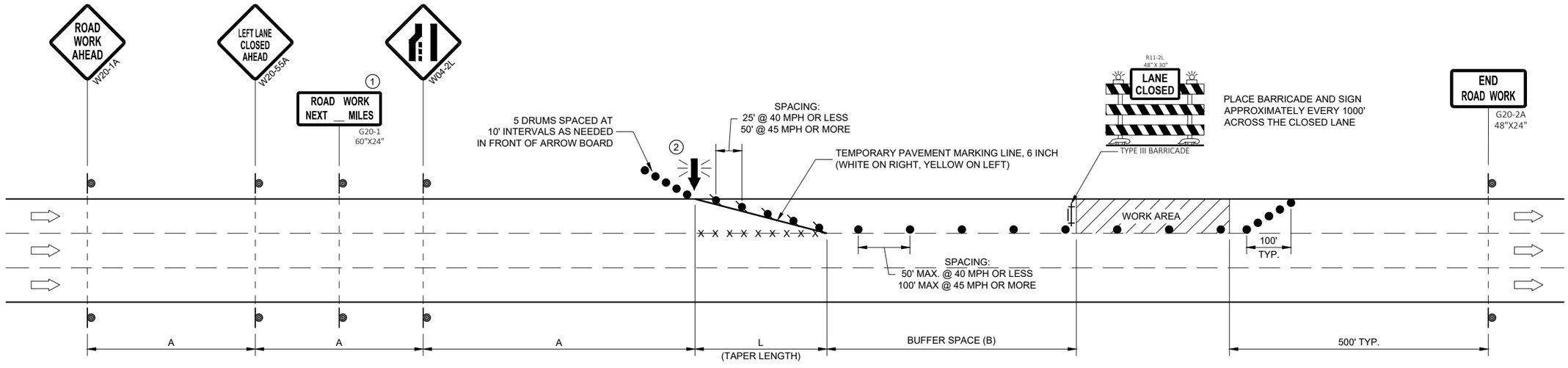
BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

(1) OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS.

(2) WHERE THE SHOULDER OR TERRACE HAS INSUFFICIENT SPACE TO PLACE THE ARROW BOARD AS SHOWN, PLACE THE ARROW BOARD AT THE END OF THE TAPER.



POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET	TAPER LENGTH (12 FT. LANE) (L) FEET	BUFFER SPACE (B) FEET
25	200'	125'	55'
30	200'	180'	85'
35	350'	245'	120'
40	350'	320'	170'
45	500'	540'	220'

TRAFFIC CONTROL, SINGLE LANE CLOSURE, DIVIDED NON-FREEWAY/EXPRESSWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEE

FHWA

6

SDD 15D20-07a

LEGEND

- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- FLASHING ARROW BOARD
- DIRECTION OF TRAFFIC
- REMOVE PAVEMENT MARKING (SEE GENERAL NOTES)
- WORK AREA

GENERAL NOTES

THIS DETAIL MAY BE USED FOR ROADWAYS WITH EITHER TWO OR THREE LANES IN EACH DIRECTION.

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36"X 36" SIGNS MAY BE USED IF APPROVED BY REGIONAL TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON COVERED OR "DOWNED" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS OR THAT WILL BE PLACED IN A CLOSED LANE MAY BE MOUNTED ON TEMPORARY SUPPORTS.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500' DESIRABLE) DISTANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE

W20-1A, G20-1 AND G20-2A SIGNS ARE NOT REQUIRED IF THE LANE CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING LINE IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARDS SO THE APPROACHING DRIVER HAS A CLEAR VIEW OF THE ARROW BOARDS AND LANE CLOSURE DRUMS FOR A MINIMUM 1500 FEET IN FRONT OF DRUMS.

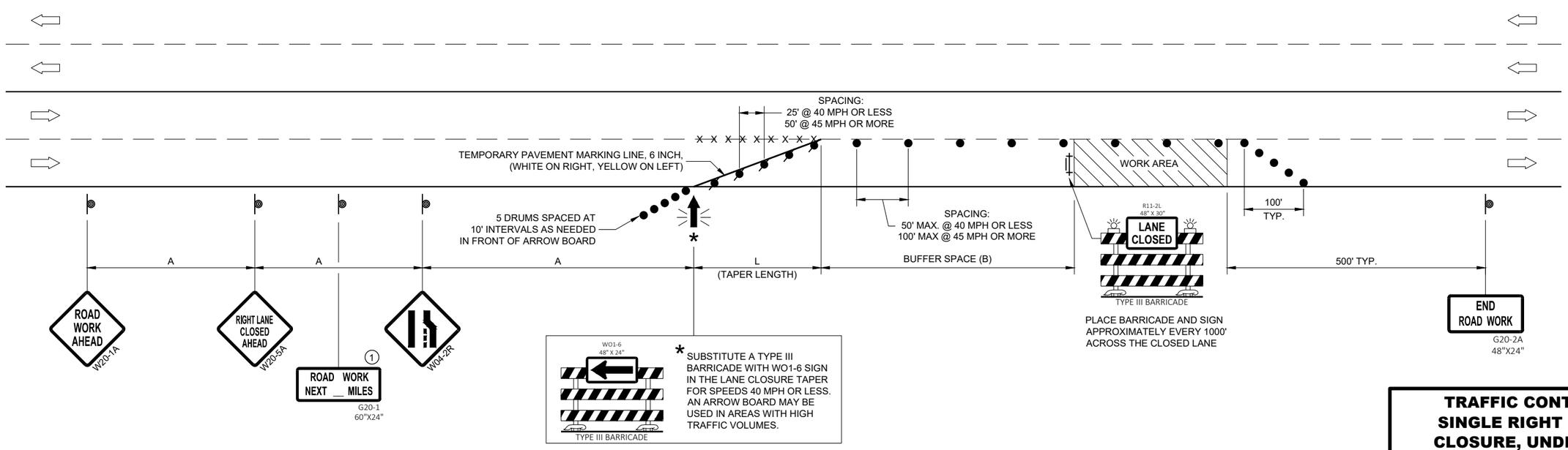
BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

(1) OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS.

POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET	TAPER LENGTH (12 FT. LANE) (L) FEET	BUFFER SPACE (B) FEET
25	200'	125'	55'
30	200'	180'	85'
35	350'	245'	120'
40	350'	320'	170'
45	500'	540'	220'
50	500'	600'	280'
55	500'	660'	335'



TRAFFIC CONTROL, SINGLE RIGHT LANE CLOSURE, UNDIVIDED NON-FREEWAY/EXPRESSWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

6

SDD 15D20-07b

LEGEND

- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- FLASHING ARROW BOARD
- DIRECTION OF TRAFFIC
- REMOVE PAVEMENT MARKING (SEE GENERAL NOTES)
- WORK AREA

GENERAL NOTES

THIS DETAIL MAY BE USED FOR ROADWAYS WITH EITHER TWO OR THREE LANES IN EACH DIRECTION.

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36"X 36" SIGNS MAY BE USED IF APPROVED BY REGIONAL TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON COVERED OR "DOWNED" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS OR THAT WILL BE PLACED IN A CLOSED LANE MAY BE MOUNTED ON TEMPORARY SUPPORTS.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500' DESIRABLE) DISTANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

W20-1A, G20-1 AND G20-2A SIGNS ARE NOT REQUIRED IF THE LANE CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING LINE IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARDS SO THE APPROACHING DRIVER HAS A CLEAR VIEW OF THE ARROW BOARDS AND LANE CLOSURE DRUMS FOR A MINIMUM 1500 FEET IN FRONT OF DRUMS.

DUE TO LACK OF SHOULDER/MEDIAN, ARROW BOARD IS PLACED AT THE THE END OF THE TAPER.

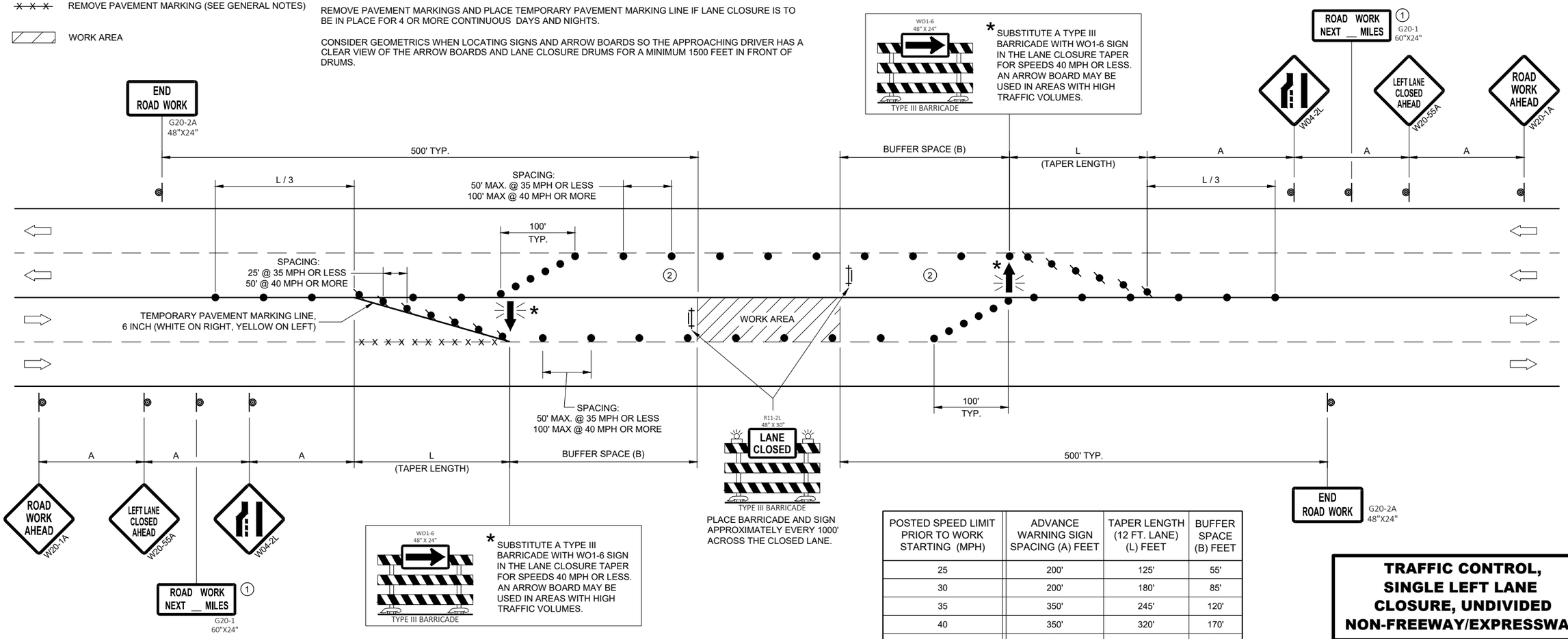
BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

(1) OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS.

(2) LANE MAY BE OPENED WHEN WORKERS ARE NOT PRESENT IN THE WORK AREA.



POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET	TAPER LENGTH (12 FT. LANE) (L) FEET	BUFFER SPACE (B) FEET
25	200'	125'	55'
30	200'	180'	85'
35	350'	245'	120'
40	350'	320'	170'
45	500'	540'	220'
50	500'	600'	280'
55	500'	660'	335'

TRAFFIC CONTROL, SINGLE LEFT LANE CLOSURE, UNDIVIDED NON-FREEWAY/EXPRESSWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023
DATE
/S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

6

SDD 15D20-07c

6

SDD 15D20-07c

LEGEND

- TRAFFIC CONTROL DRUM
- SIGN ON PERMANENT SUPPORT
- DIRECTION OF TRAFFIC
- FLASHING ARROW BOARD
- WORK AREA

GENERAL NOTES

THIS DETAIL IS TYPICAL FOR CLOSING THE RIGHT SHOULDER. FOR CLOSING THE LEFT SHOULDER, REVERSE THE TRAFFIC CONTROL.

THIS DETAIL MAY BE USED FOR DIVIDED ROADWAYS WITH ANY NUMBER OF TRAVEL LANES.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE FHWA'S MANUAL OF STANDARD HIGHWAY SIGNS OR THE WISCONSIN STANDARD SIGN PLATES.

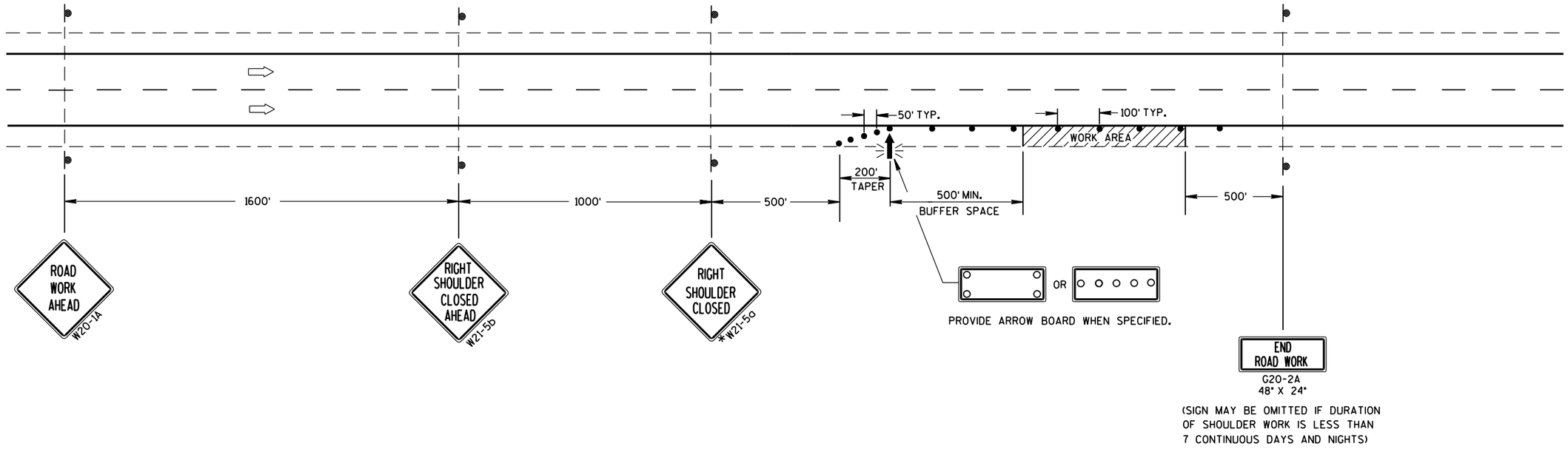
SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

CHANNELIZING DEVICES PLACED ADJACENT TO THE WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

WHEN A RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

*FOR SHORT DURATION SHOULDER WORK OF LESS THAN ONE HOUR, THE W21-5a SIGN MAY BE OMITTED.



TRAFFIC CONTROL SHOULDER CLOSURE ON DIVIDED ROADWAY, SPEEDS GREATER THAN 40 MPH

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

June 2016 — DATE

/S/ Peter Amakobe Atepe
STATEWIDE WORK ZO[illegible] SAFETY ENGI[illegible]

FHWA

6

S.D.D. 15 D 27-3

LEGEND

SIGN ON PERMANENT SUPPORT

TRAFFIC CONTROL DRUM

DIRECTION OF TRAFFIC

WORK ZONE

GENERAL NOTES

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" SIGNS MAY BE USED IF APPROVED BY THE REGIONAL TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

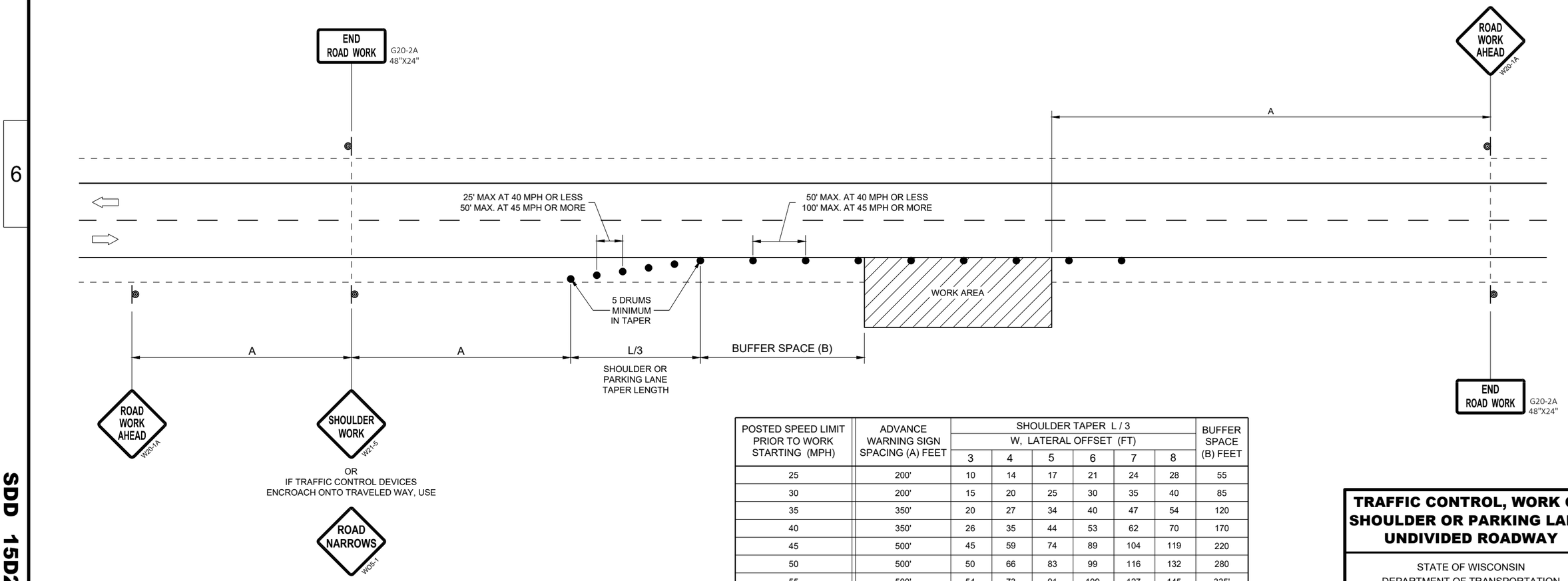
ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

W20-1A AND G20-2A SIGNS ARE NOT REQUIRED IF THE WORK AREA IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT. G20-2A SIGNS MAY ALSO BE OMITTED IF DURATION OF WORK IS LESS THAN 7 CONTINUOUS DAYS AND NIGHTS.



POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET	SHOULDER TAPER L / 3 W, LATERAL OFFSET (FT)						BUFFER SPACE (B) FEET
		3	4	5	6	7	8	
25	200'	10	14	17	21	24	28	55
30	200'	15	20	25	30	35	40	85
35	350'	20	27	34	40	47	54	120
40	350'	26	35	44	53	62	70	170
45	500'	45	59	74	89	104	119	220
50	500'	50	66	83	99	116	132	280
55	500'	54	73	91	109	127	145	335'

TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2020
DATE

/S/ Andrew Heidtke
STATEWIDE WORK ZONE T SAFETY ENGINEER

FHWA

SDD 15D28 - 04

SDD 15D28 - 04

GENERAL NOTES

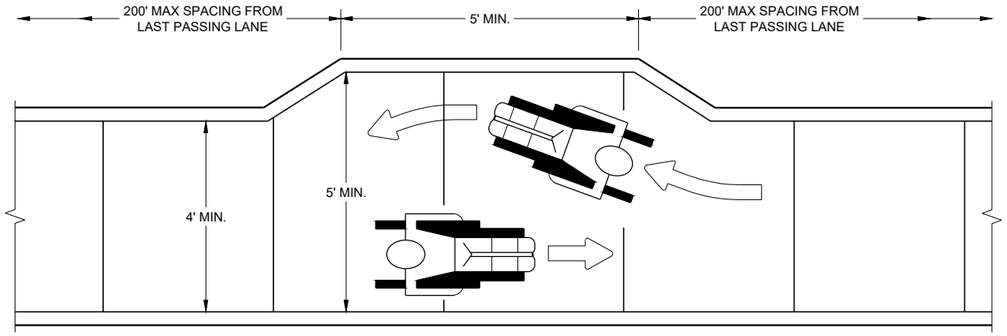
BARRICADE DEVICE SELECTED FROM APPROVED PRODUCT LIST

(1) REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.

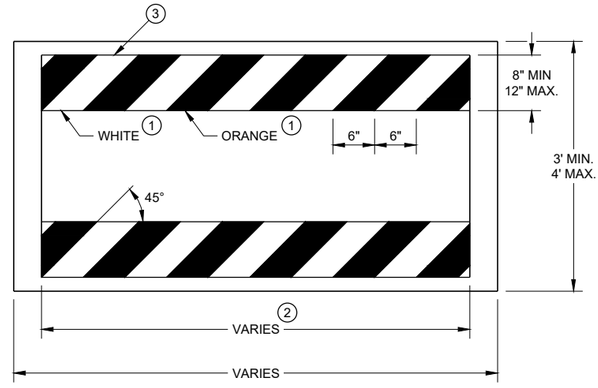
(2) SHEETING REQUIRED ON MORE THAN 50% OF BARRICADE WIDTH.

(3) PLACE SHEETING ON BOTH SIDES OF THE BARRICADE.

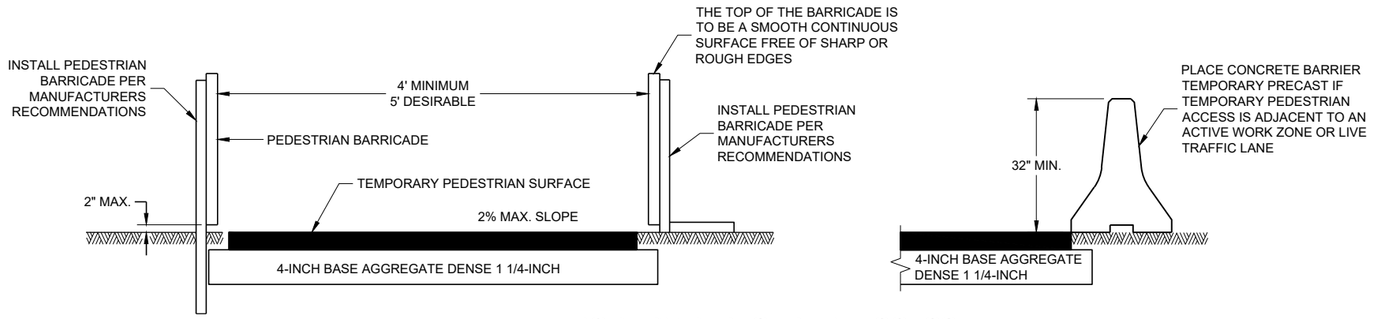
* USE THIS DETAIL FOR SHEETING PLACEMENT REFERENCE.



NARROW SIDEWALK PASSING DETAIL



TEMPORARY PEDESTRIAN BARRICADE*

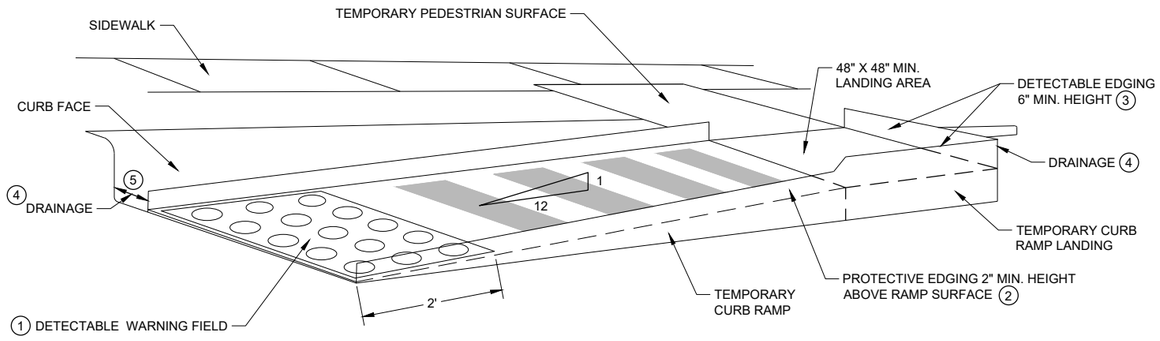


TEMPORARY PEDESTRIAN ACCESS

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION

6

SDD 15D30 - 09a



PERSPECTIVE VIEW

GENERAL NOTES

CURB RAMPS SHALL BE 48" MIN. WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE.

CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.

CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.
LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN ½" WIDTH.

CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED ½". LATERAL EDGES MAY BE VERTICAL UP TO ¼" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN ¼" AND ½".

(1) INSTALL CONTRASTING TEMPORARY DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS, AS SHOWN IN THE PLANS.

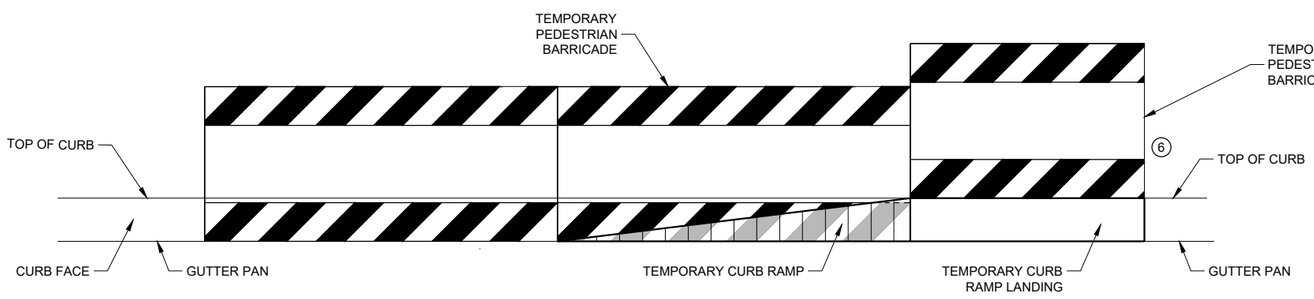
(2) PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.

(3) DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).

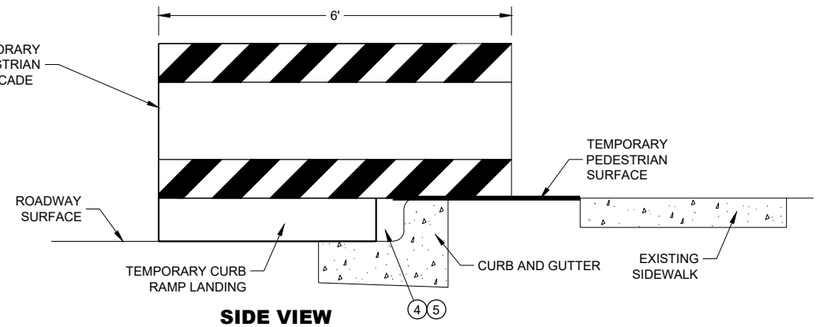
(4) DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.

(5) ENSURE CURB RAMP IS OUT OF THE GUTTER PAN.

(6) IF ONLY PART OF THE END PANEL OF TEMPORARY PEDESTRIAN BARRICADE PANEL IS NEEDED, EXTEND EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL HERE.



FRONT VIEW



SIDE VIEW

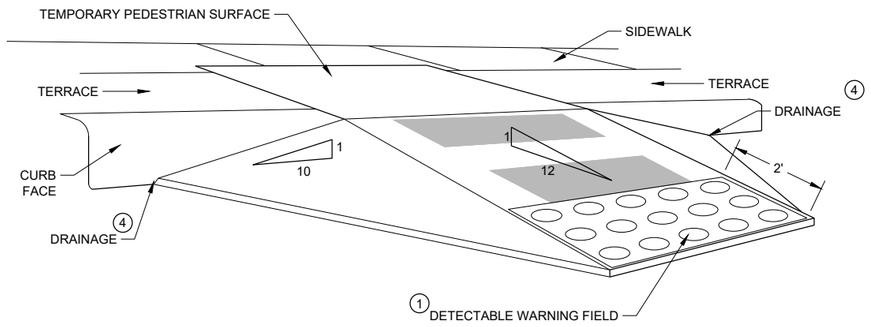
TEMPORARY CURB RAMP PARALLEL TO CURB

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION

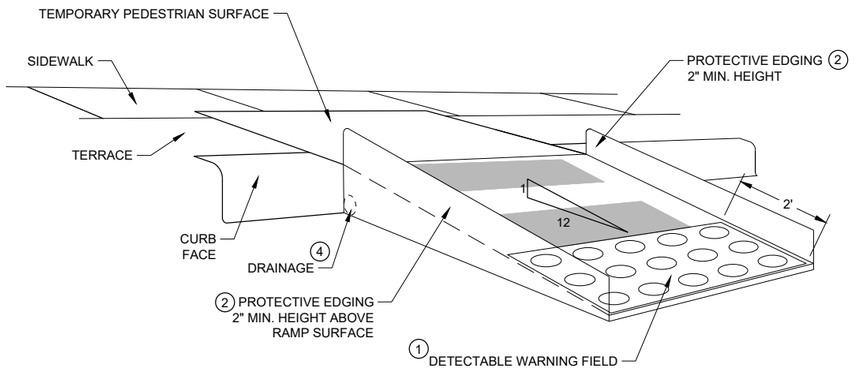
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

SDD 15D30 - 09b



WITH SIDE APRON (5)



WITH PROTECTIVE EDGE

TEMPORARY CURB RAMP PERPENDICULAR TO CURB

GENERAL NOTES

CURB RAMPS SHALL BE 48" MINIMUM WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE.

ALTERNATE SIDEWALK WORK BETWEEN LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.

CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.

CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.

LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN ½" WIDTH.

CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED ½". LATERAL EDGES MAY BE VERTICAL UP TO ¼" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN ¼" AND ½".

(1) INSTALL CONTRASTING TEMPORARY DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS, AS SHOWN IN THE PLANS

(2) PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.

(3) DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).

(4) DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.

(5) CAN ONLY BE USED FOR RAMPS WITH 6" OR LESS OF VERTICAL CHANGE.

6

SDD 15D30 - 09c

GENERAL NOTES

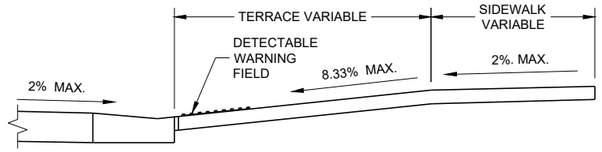
BARRICADE DEVICE SELECTED FROM APPROVED PRODUCT LIST

(1) REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.

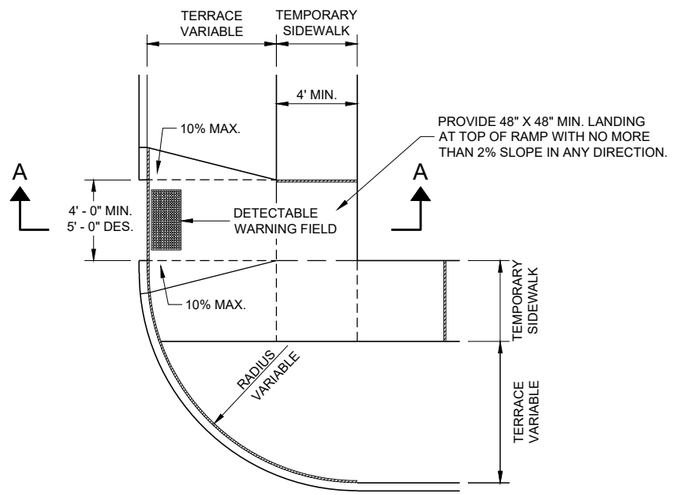
(2) SHEETING REQUIRED ON MORE THAN 50% OF BARRICADE WIDTH.

(3) PLACE SHEETING ON BOTH SIDES OF THE BARRICADE.

* USE THIS DETAIL FOR SHEETING PLACEMENT REFERENCE.



SECTION A - A



PLAN VIEW

TEMPORARY TYPE 3 RAMP

(OUTSIDE OF CROSSWALK AREA)

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEE

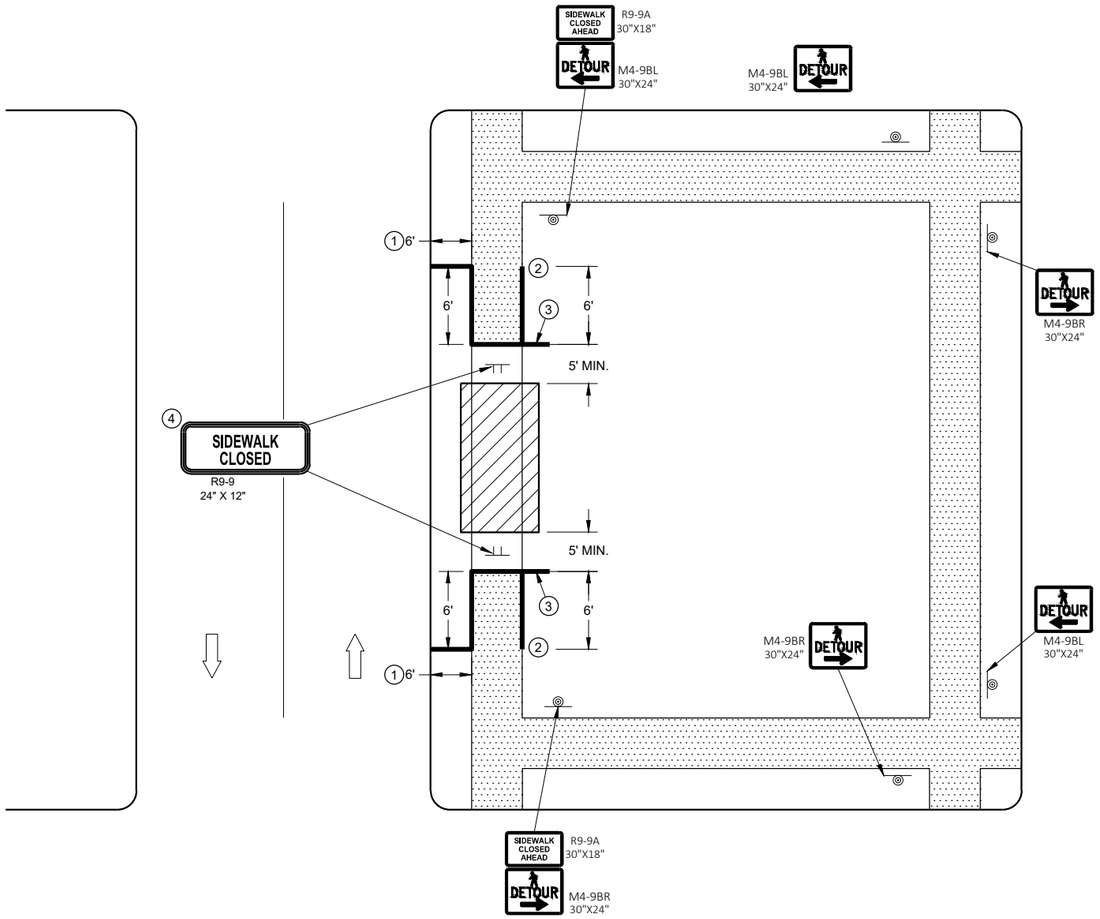
FHWA

6

SDD 15D30-09d

LEGEND

- SIGN ON PERMANENT SUPPORT
- SIGN ON TEMPORARY SUPPORT
- UNDER PEDESTRIAN TRAFFIC
- WORK AREA
- TEMPORARY PEDESTRIAN BARRICADE
- DIRECTION OF TRAFFIC



GENERAL NOTES

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

PLACE TEMPORARY PEDESTRIAN BARRICADE TO FIT FIELD CONDITIONS, AVOIDING CONFLICTS WITH DRIVEWAYS AND OTHER EXISTING FEATURES.

1. IF TERRACE IS LESS THAN 6 FEET WIDE, OMIT TEMPORARY PEDESTRIAN BARRICADE FROM THE SIDEWALK TO THE CURB.
2. PLACE BARRICADE CLOSURE SO THAT THE TEMPORARY PEDESTRIAN BARRICADE END IS AT THE LAST OPEN SIDEWALK ACCESS TO RESIDENCES OR BUSINESSES BEFORE THE SIDEWALK CLOSURE.
3. IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
4. MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.

SIDEWALK DETOUR, SIDEWALK ONLY ON ONE SIDE

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION

6

SDD 15D30 - 09f

LEGEND

SIGN ON PERMANENT SUPPORT

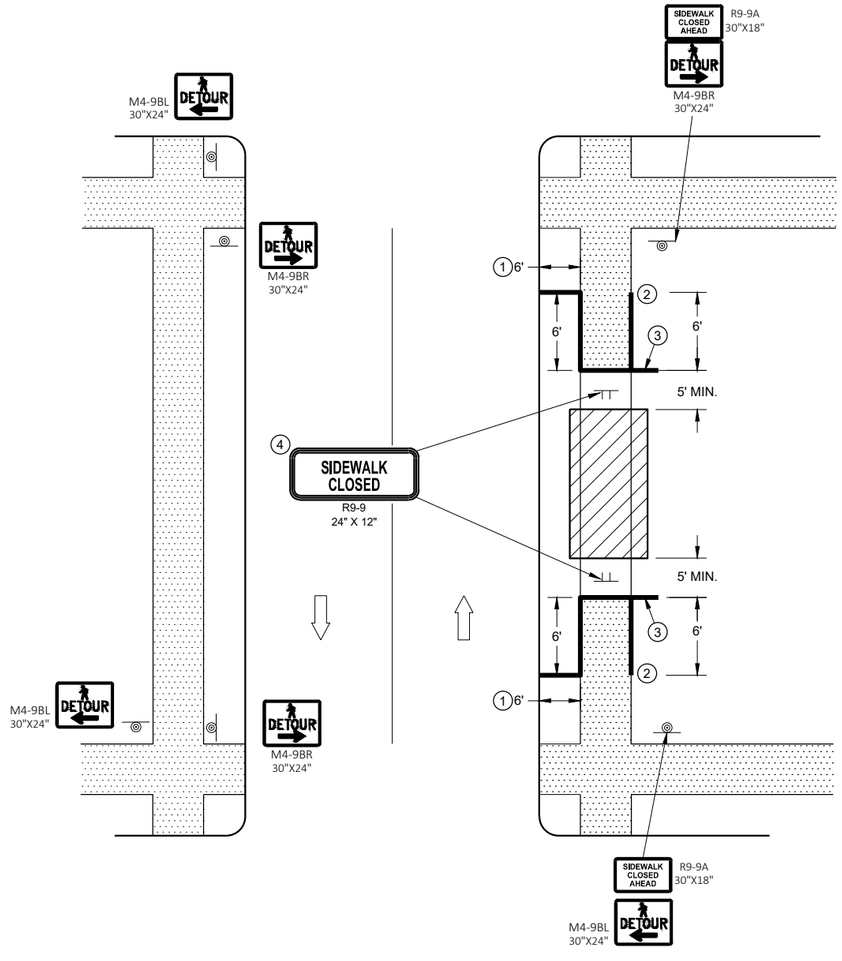
SIGN ON TEMPORARY SUPPORT

UNDER PEDESTRIAN TRAFFIC

WORK AREA

TEMPORARY PEDESTRIAN BARRICADE

DIRECTION OF TRAFFIC



GENERAL NOTES

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

PLACE TEMPORARY PEDESTRIAN BARRICADE TO FIT FIELD CONDITIONS, AVOIDING CONFLICT WITH DRIVEWAYS AND OTHER EXISTING FEATURES.

1. IF TERRACE IS LESS THAN 6 FEET WIDE, OMIT TEMPORARY PEDESTRIAN BARRICADE FROM THE SIDEWALK TO THE CURB.
2. PLACE BARRICADE CLOSURE SO THAT THE TEMPORARY PEDESTRIAN BARRICADE END IS AT THE LAST OPEN SIDEWALK ACCESS TO RESIDENCES OR BUSINESSES BEFORE THE SIDEWALK CLOSURE.
3. IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
4. MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.

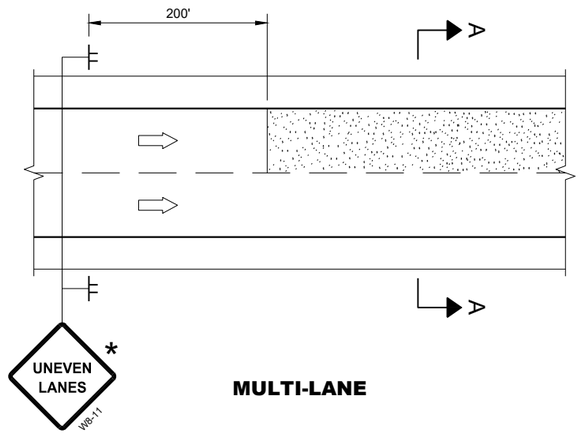
SIDEWALK DETOUR, SIDEWALK ON BOTH SIDES

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION

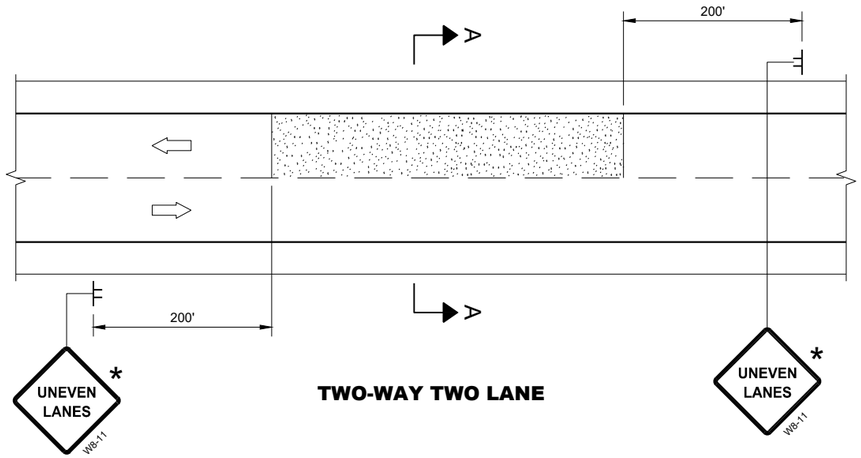
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

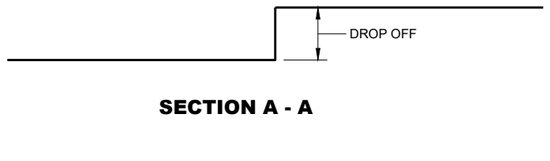
SDD 15D30 - 09k



MULTI-LANE

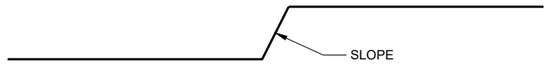


TWO-WAY TWO LANE

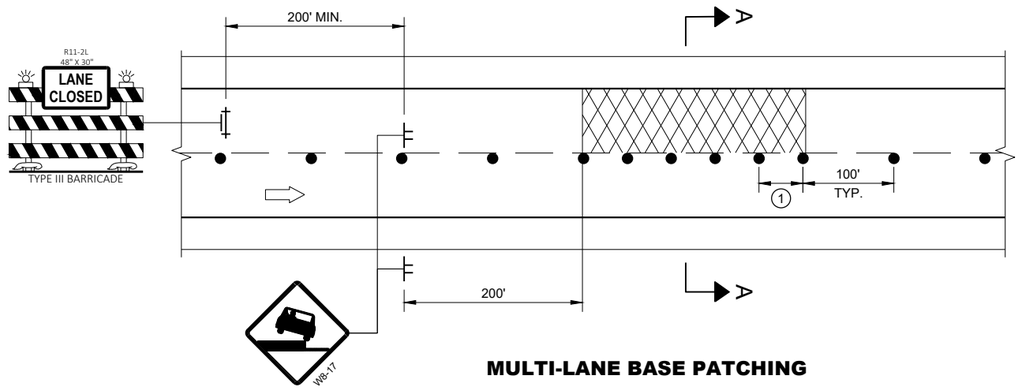


SECTION A - A

OR

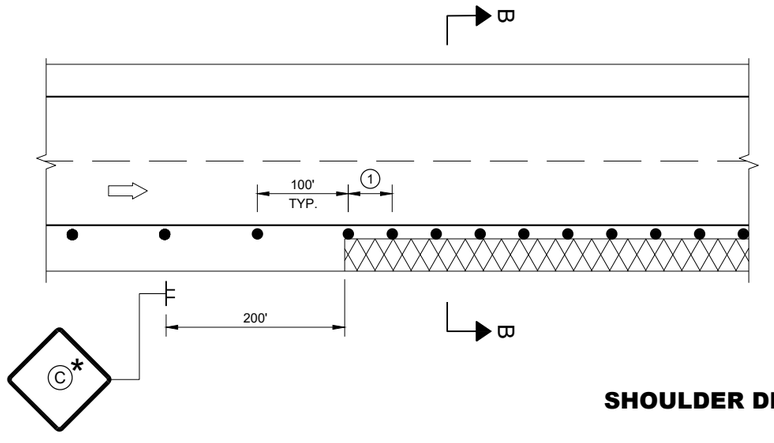


SECTION A - A

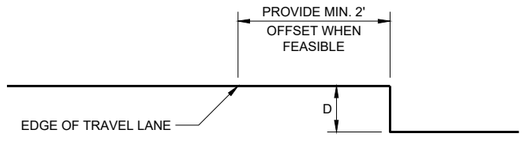


MULTI-LANE BASE PATCHING

ADJACENT LANE DROP-OFFS



SHOULDER DROP-OFFS



SECTION B - B

D	SIGN (C)
< 2" WITH A SLOPE STEEPER THAN 3:1	LOW SHOULDER WO8-9
2" < 6" WITH A SLOPE STEEPER THAN 3:1	SHOULDER DROP - OFF W8-9A PROVIDE A 3:1 OR FLATTER SLOPE OF MATERIAL ADJACENT TO THE PAVEMENT

GENERAL NOTES

FOR SPOT LOCATIONS USE ENGINEERING JUDGEMENT WHEN PLACING ADDITIONAL SIGNS.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

* IF THE DROP-OFF IS CONTINUOUS ALONG THE PROJECT, PLACE ADDITIONAL SIGNS EVERY 1 MILE AND AFTER EVERY ENTRANCE RAMP.

(1) USE CLOSER SPACING WHEN DELINEATING DROP-OFF.

LEGEND

- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL DRUM
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- DIRECTION OF TRAFFIC
- WORK AREA WITH DROP-OFF
- MILLED SURFACE

TRAFFIC CONTROL, DROP-OFF SIGNING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
March 2018 — DATE
/S/ Andrew Heidtke — WORK ZONE ENGINEE

FHWA

6

SDD 15D39 - 02

EASTBOUND USH 14 EARTHWORK SUMMARY

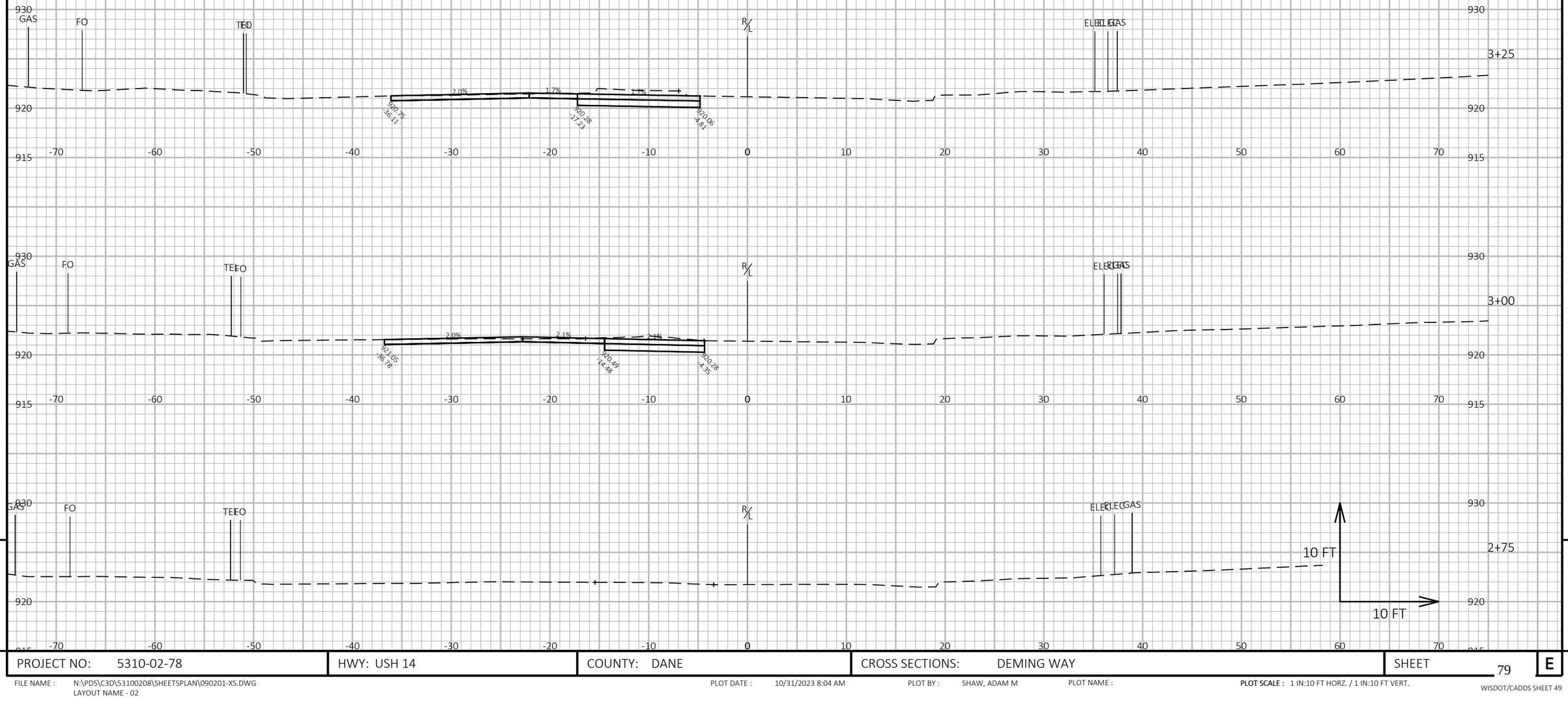
STATION	AREA (SF)		INCREMENTAL VOL (CY)		CUMULATIVE VOL (CY)		
	CUT	FILL	CUT	FILL	CUT	UNEXPANDED FILL	MASS ORDINATE (CUT - FILL)
388+25.00	0	0	0	0	0	0	0
388+50.00	18	0	9	0	9	0	9
388+75.00	18	0	17	0	26	0	26
389+00.00	18	0	17	0	43	0	43
389+25.00	20	0	18	0	61	0	61
389+50.00	23	0	20	0	81	0	81
389+75.00	27	0	23	0	104	0	104
390+00.00	33	0	28	0	132	0	132
390+25.00	29	0	28	0	160	0	160
390+50.00	28	0	26	0	186	0	186
390+75.00	29	0	26	0	212	0	212
391+00.00	32	1	28	1	240	1	239
391+25.00	32	1	30	1	270	2	268
391+50.00	32	1	30	1	300	3	297
391+75.00	35	1	31	1	331	4	327
392+00.00	34	0	32	0	363	4	359
392+25.00	35	0	32	0	395	4	391
392+50.00	38	0	34	0	429	4	425

WESTBOUND USH 14 EARTHWORK SUMMARY

STATION	AREA (SF)		INCREMENTAL VOL (CY)		CUMULATIVE VOL (CY)		
	CUT	FILL	CUT	FILL	CUT	UNEXPANDED FILL	MASS ORDINATE (CUT - FILL)
401+00.00	32.15	0.00	0	0	0	0	0
401+25.00	32.66	0.00	30	0	30	0	30
401+50.00	34.33	0.00	31	0	61	0	61
401+75.00	33.59	0.00	31	0	92	0	92
402+00.00	33.31	0.00	31	0	123	0	123
402+25.00	32.75	0.00	31	0	154	0	154
402+50.00	30.99	0.00	30	0	184	0	184
402+75.00	30.94	0.00	29	0	213	0	213
403+00.00	30.72	0.00	29	0	242	0	242
403+25.00	30.70	0.00	28	0	270	0	270
403+50.00	30.39	0.00	28	0	298	0	298
403+75.00	30.60	0.00	28	0	326	0	326
404+00.00	29.99	0.00	28	0	354	0	354
404+25.00	29.87	0.00	28	0	382	0	382
404+50.00	29.08	0.00	27	0	409	0	409
404+75.00	27.91	0.00	26	0	435	0	435
405+00.00	28.55	0.00	26	0	461	0	461
405+25.00	27.78	0.47	26	0	487	0	487
405+50.00	32.44	0.00	28	0	515	0	515
405+75.00	34.66	0.00	31	0	546	0	546
406+00.00	42.59	0.00	36	0	582	0	582

DEMING WAY (D-LINE) EARTHWORK SUMMARY

STATION	DISTANCE	AREA (SF)		INCREMENTAL VOL (CY)		CUMULATIVE VOL (CY)		
		CUT	FILL	CUT	FILL	CUT	UNEXPANDED FILL	MASS ORDINATE (CUT - FILL)
2+75.00 D	0.00	0	0	0	0	0	0	0
3+00.00 D	25.00	14	0	6	0	6	0	6
3+25.00 D	25.00	19	0	15	0	21	0	21
3+50.00 D	25.00	22	0	19	0	40	0	40
3+75.00 D	25.00	25	0	22	0	62	0	62
4+00.00 D	25.00	21	0	21	0	83	0	83
4+25.00 D	25.00	20	0	19	0	102	0	102
4+50.00 D	25.00	0	0	9	0	111	0	111



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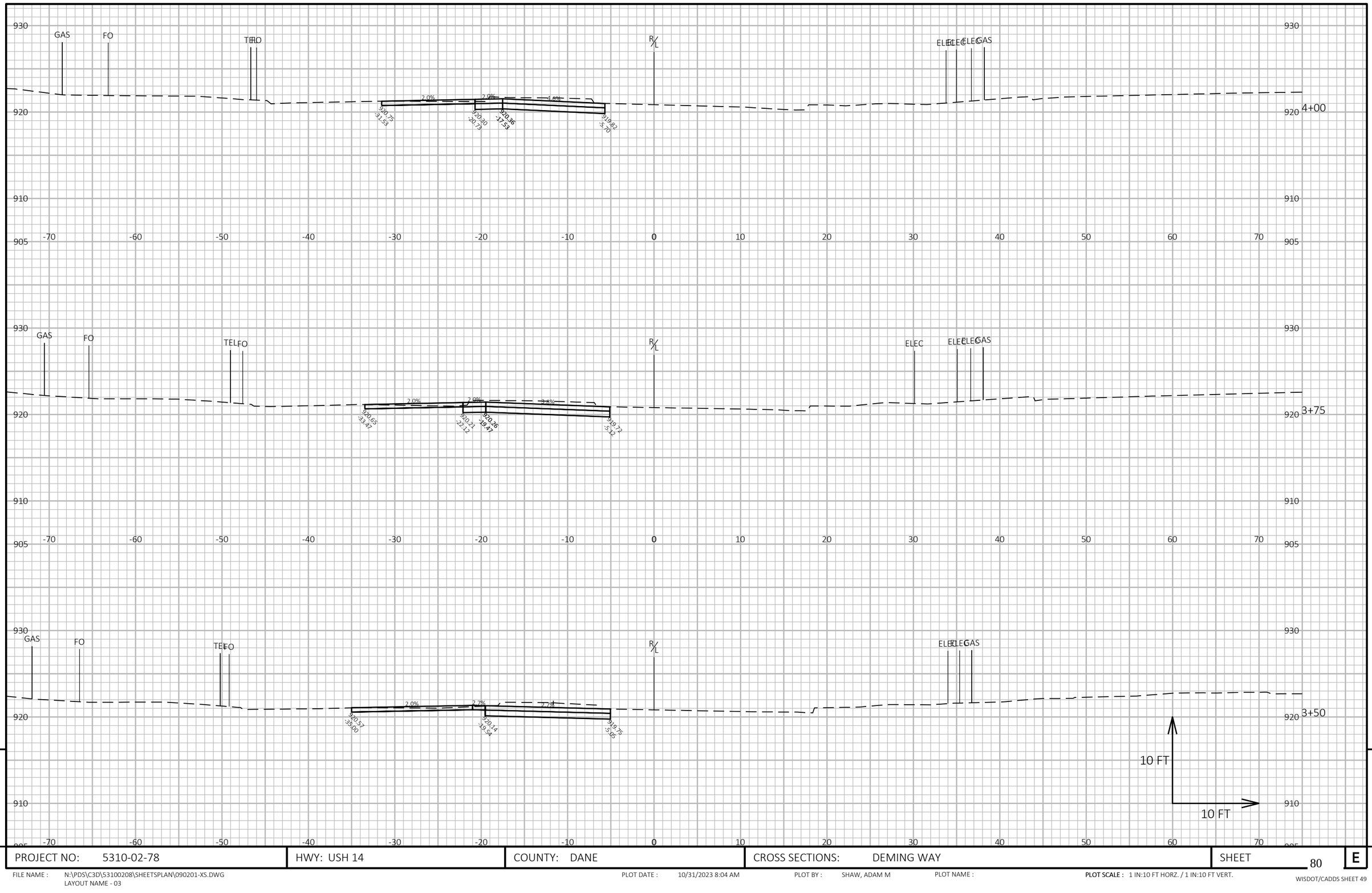
PROJECT NO: 5310-02-78	HWY: USH 14	COUNTY: DANE	CROSS SECTIONS: DEMING WAY	SHEET 79	E

FILE NAME : N:\PDS\C3D\53100208\SHEETSPLAN\090201-XS.DWG
LAYOUT NAME - 02

PLOT DATE : 10/31/2023 8:04 AM PLOT BY : SHAW, ADAM M PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



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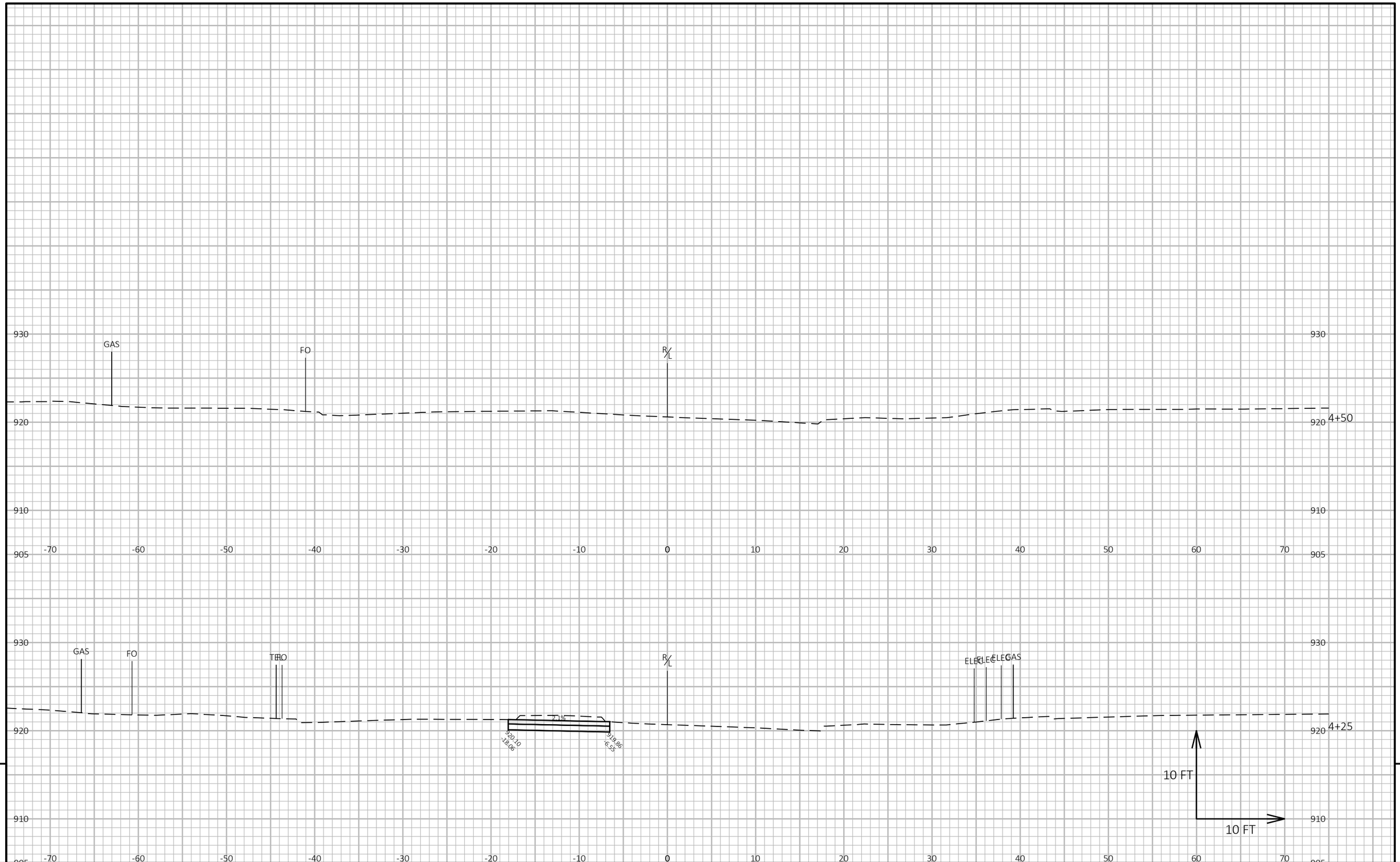
PROJECT NO: 5310-02-78	HWY: USH 14	COUNTY: DANE	CROSS SECTIONS: DEMING WAY	SHEET 80	E

FILE NAME : N:\PDS\C3D\53100208\SHEETSPLAN\090201-XS.DWG
LAYOUT NAME - 03

PLOT DATE : 10/31/2023 8:04 AM PLOT BY : SHAW, ADAM M PLOT NAME : **PLOT SCALE :** 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



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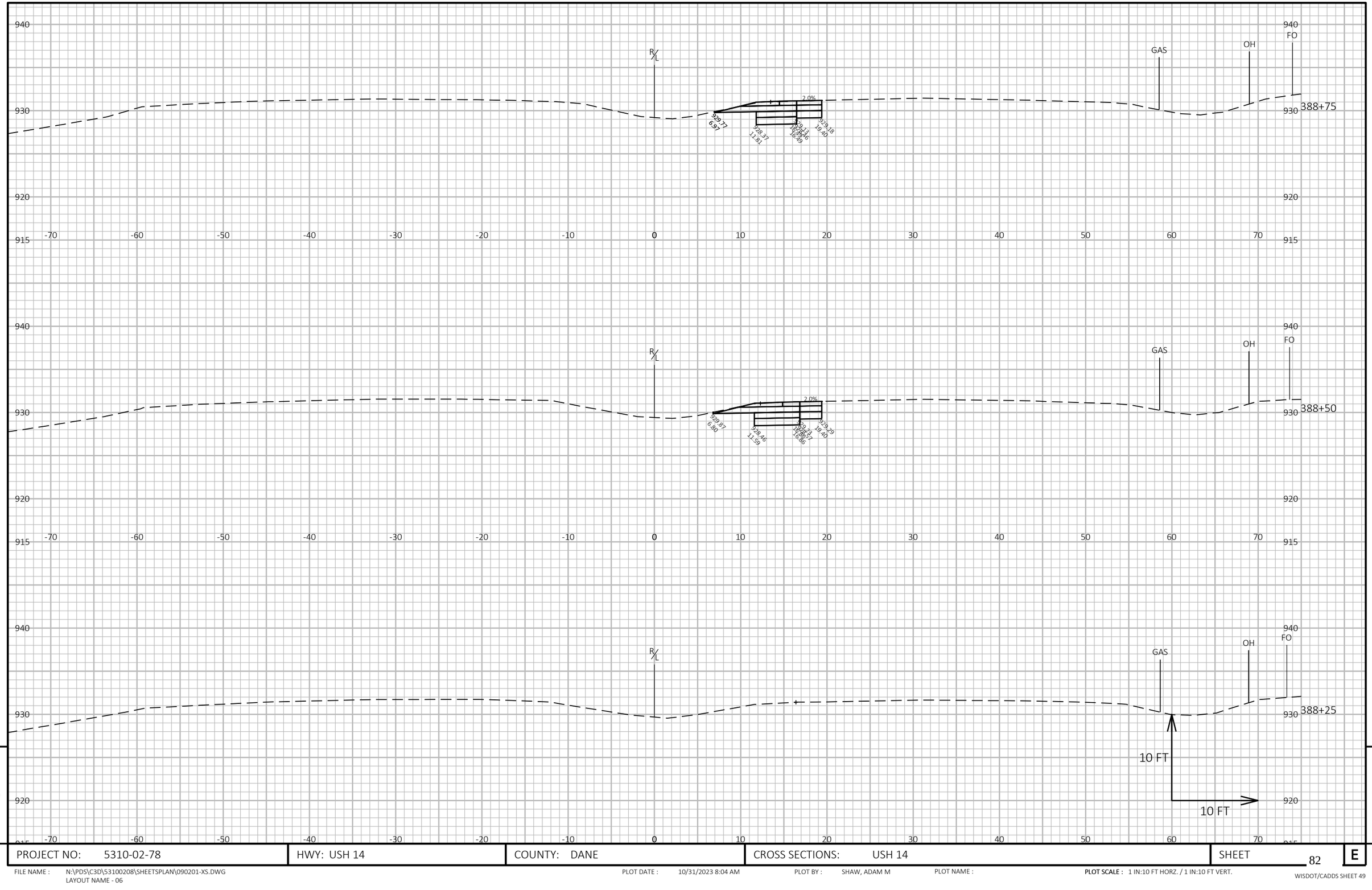
PROJECT NO: 5310-02-78	HWY: USH 14	COUNTY: DANE	CROSS SECTIONS: DEMING WAY	SHEET 81	E

FILE NAME : N:\PDS\C3D\53100208\SHEETSPLAN\090201-XS.DWG
LAYOUT NAME - 04

PLOT DATE : 10/31/2023 8:04 AM PLOT BY : SHAW, ADAM M PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



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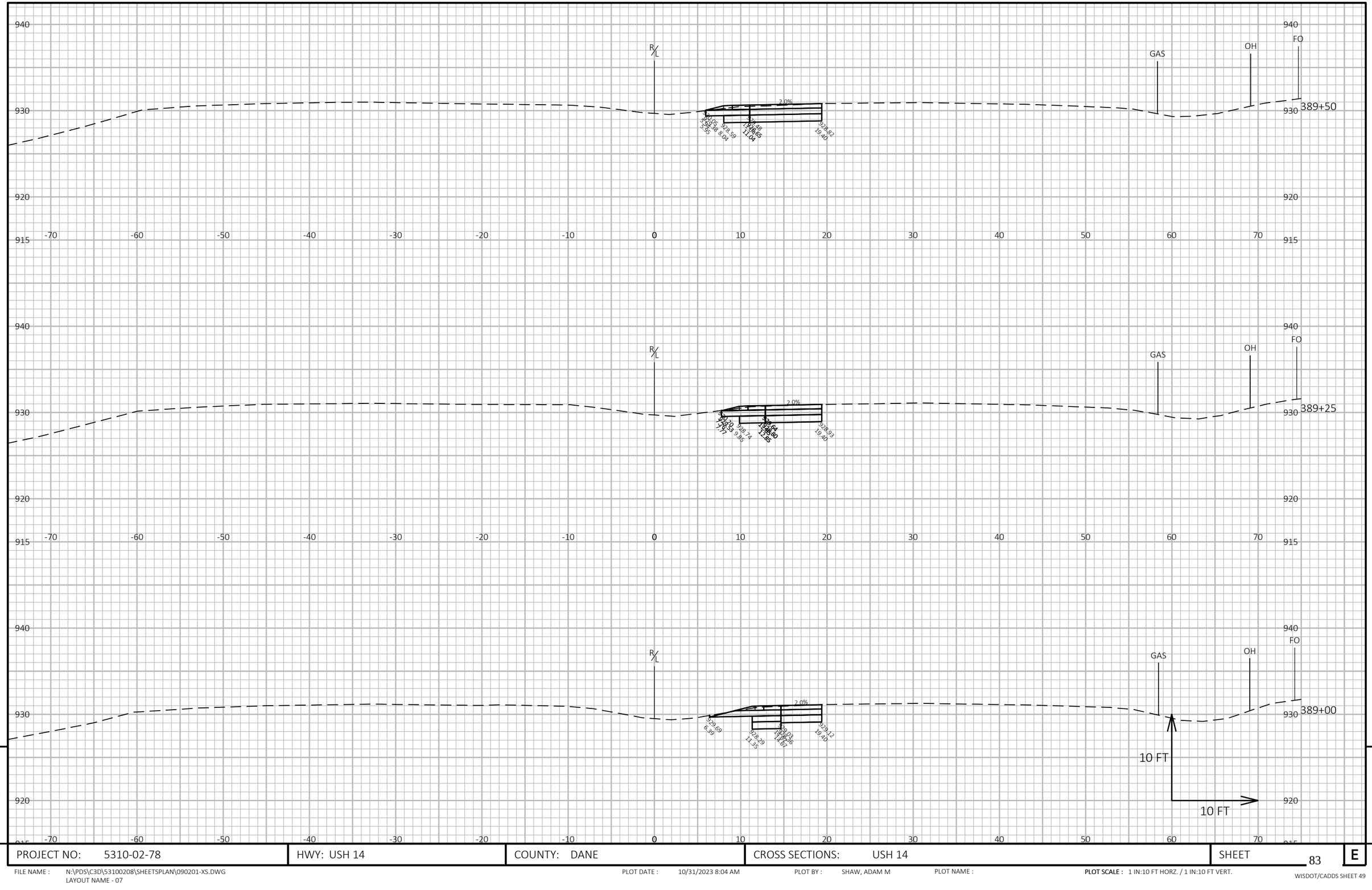
PROJECT NO: 5310-02-78	HWY: USH 14	COUNTY: DANE	CROSS SECTIONS: USH 14	SHEET 82	E

FILE NAME : N:\PDS\C3D\53100208\SHEETSPLAN\090201-XS.DWG
LAYOUT NAME - 06

PLOT DATE : 10/31/2023 8:04 AM PLOT BY : SHAW, ADAM M PLOT NAME : PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.



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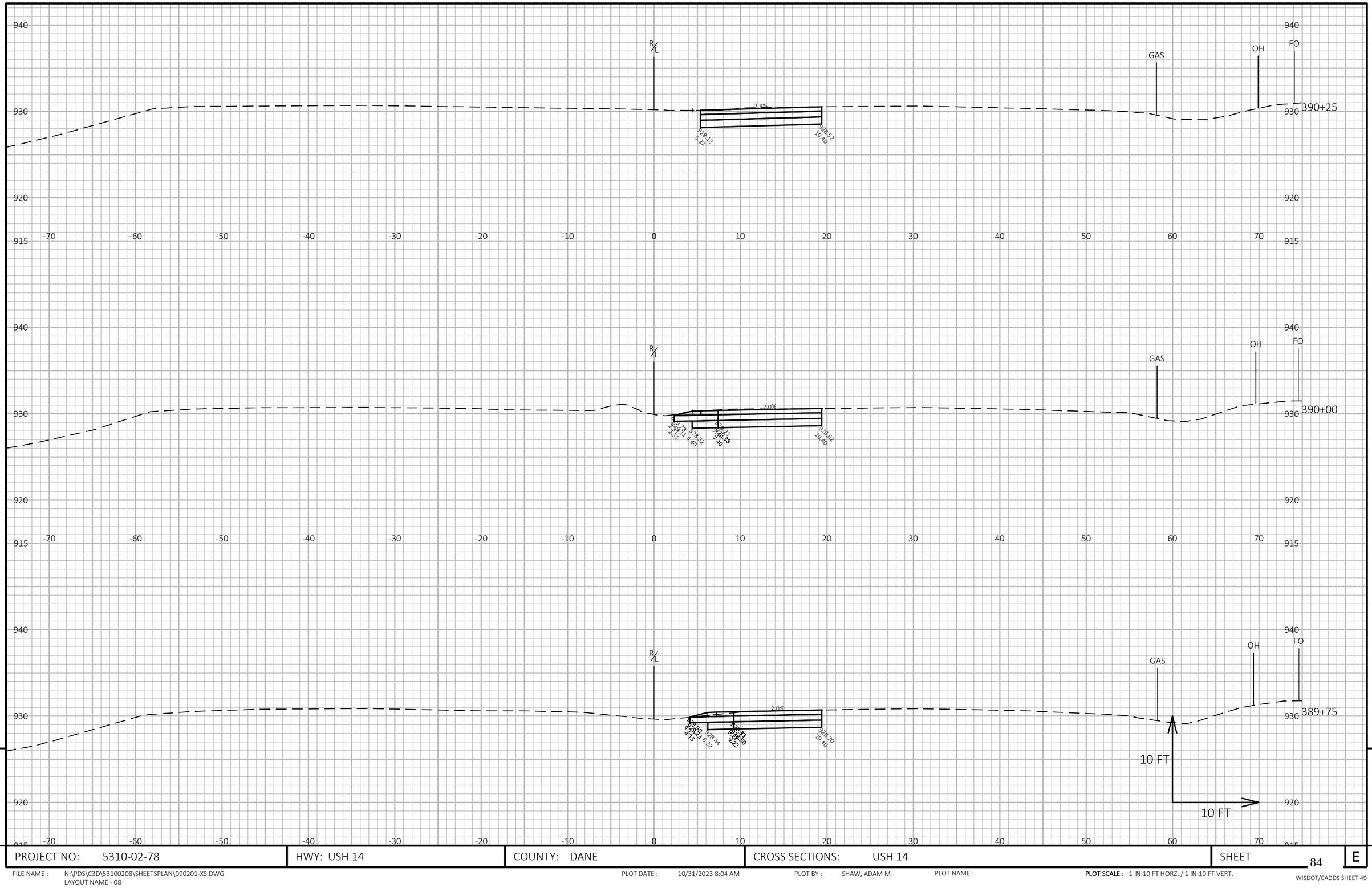
PROJECT NO: 5310-02-78	HWY: USH 14	COUNTY: DANE	CROSS SECTIONS: USH 14	SHEET 83	E

FILE NAME : N:\PDS\C3D\53100208\SHEETSPLAN\090201-XS.DWG
LAYOUT NAME - 07

PLOT DATE : 10/31/2023 8:04 AM PLOT BY : SHAW, ADAM M PLOT NAME : PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.



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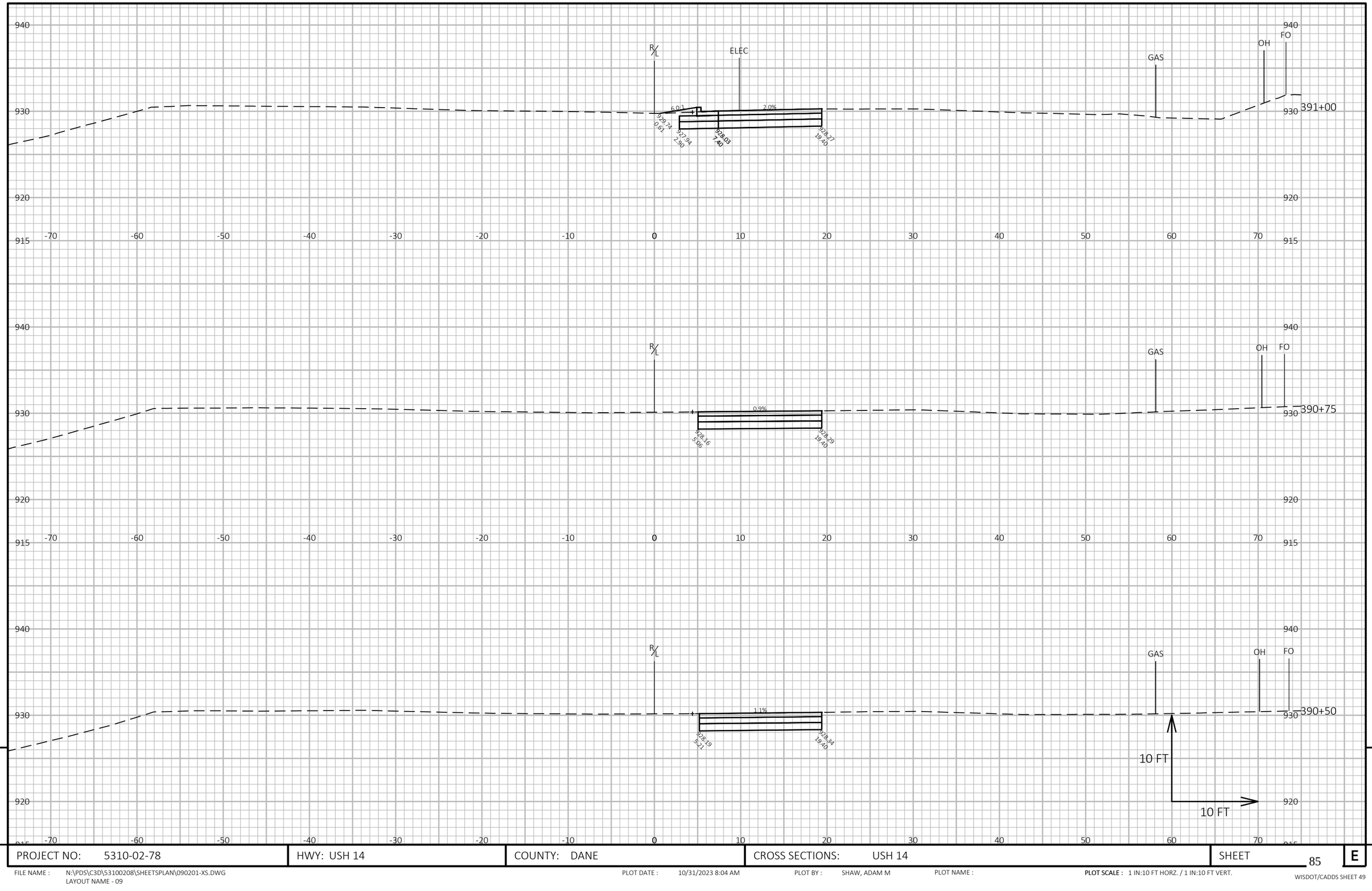
PROJECT NO: 5310-02-78	HWY: USH 14	COUNTY: DANE	CROSS SECTIONS: USH 14	SHEET 84	E

FILE NAME : N:\PDS\C3D\53100208\SHEETSPLAN\090201-XS.DWG
LAYOUT NAME - 08

PLOT DATE : 10/31/2023 8:04 AM PLOT BY : SHAW, ADAM M PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



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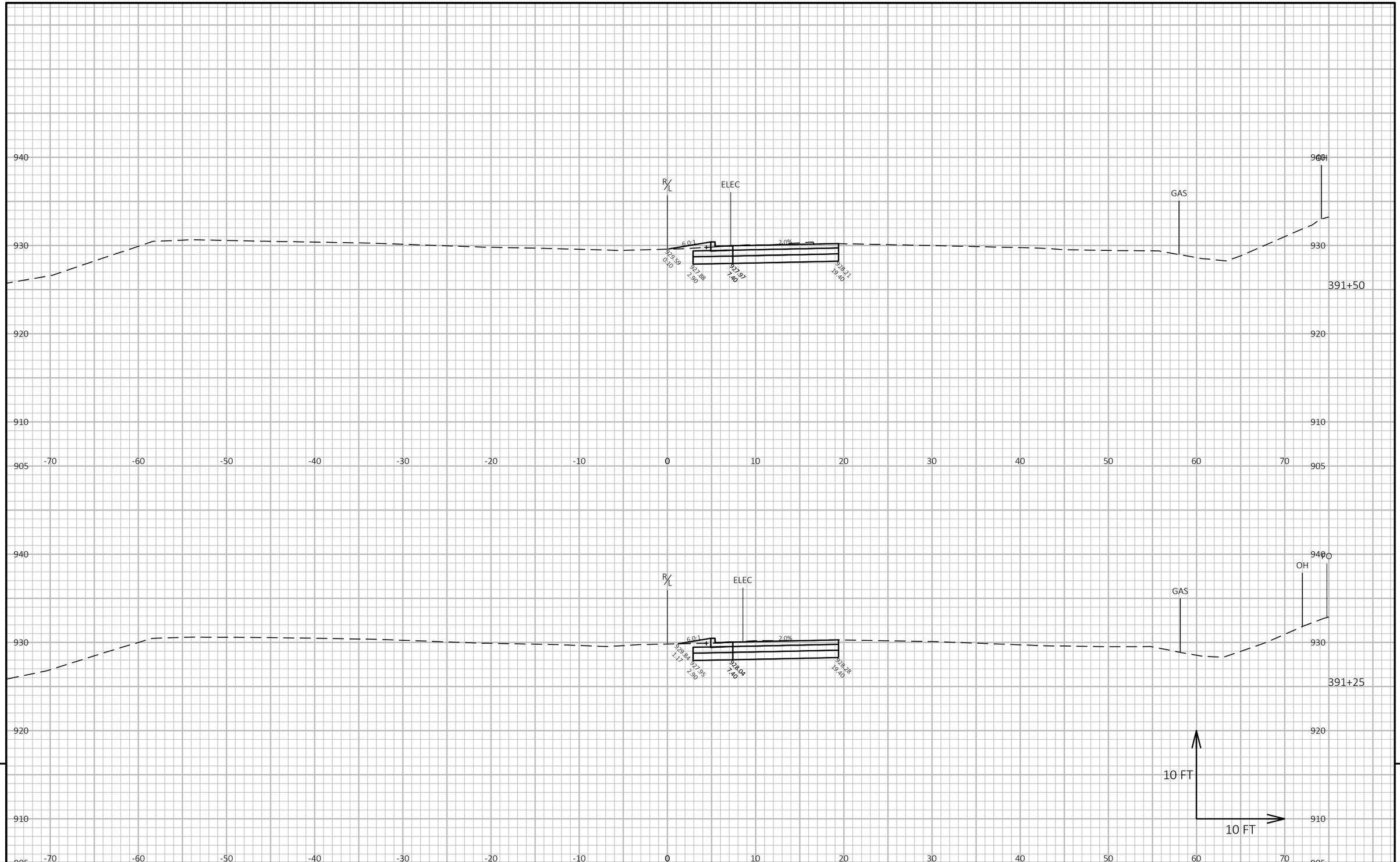
PROJECT NO: 5310-02-78	HWY: USH 14	COUNTY: DANE	CROSS SECTIONS: USH 14	SHEET 85	E

FILE NAME : N:\PDS\C3D\53100208\SHEETSPLAN\090201-XS.DWG
LAYOUT NAME - 09

PLOT DATE : 10/31/2023 8:04 AM PLOT BY : SHAW, ADAM M PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



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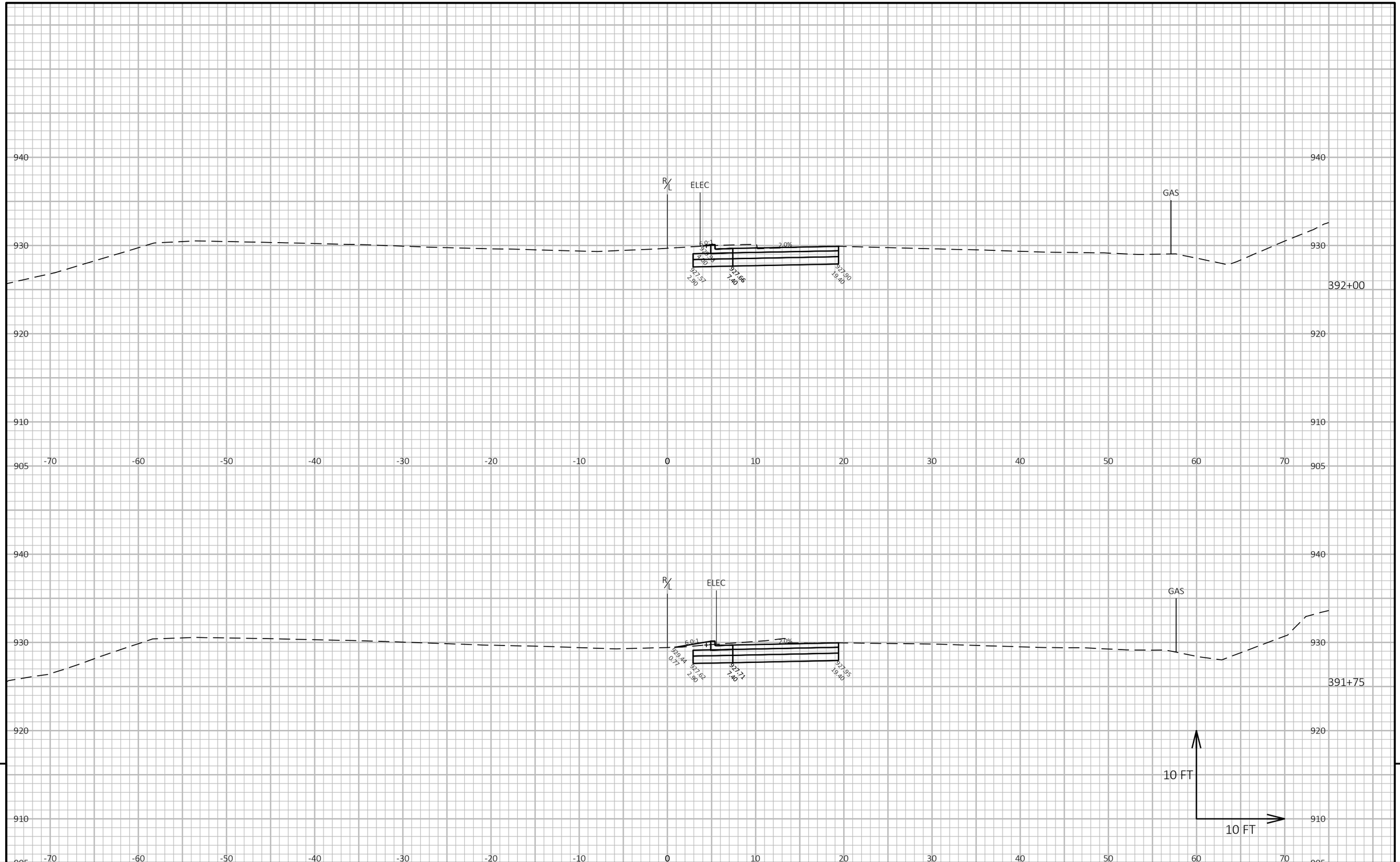
PROJECT NO: 5310-02-78	HWY: USH 14	COUNTY: DANE	CROSS SECTIONS: USH 14	SHEET 86	E

FILE NAME : N:\PDS\C3D\53100208\SHEETSPLAN\090201-XS.DWG
LAYOUT NAME - 10

PLOT DATE : 10/31/2023 8:04 AM PLOT BY : SHAW, ADAM M PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



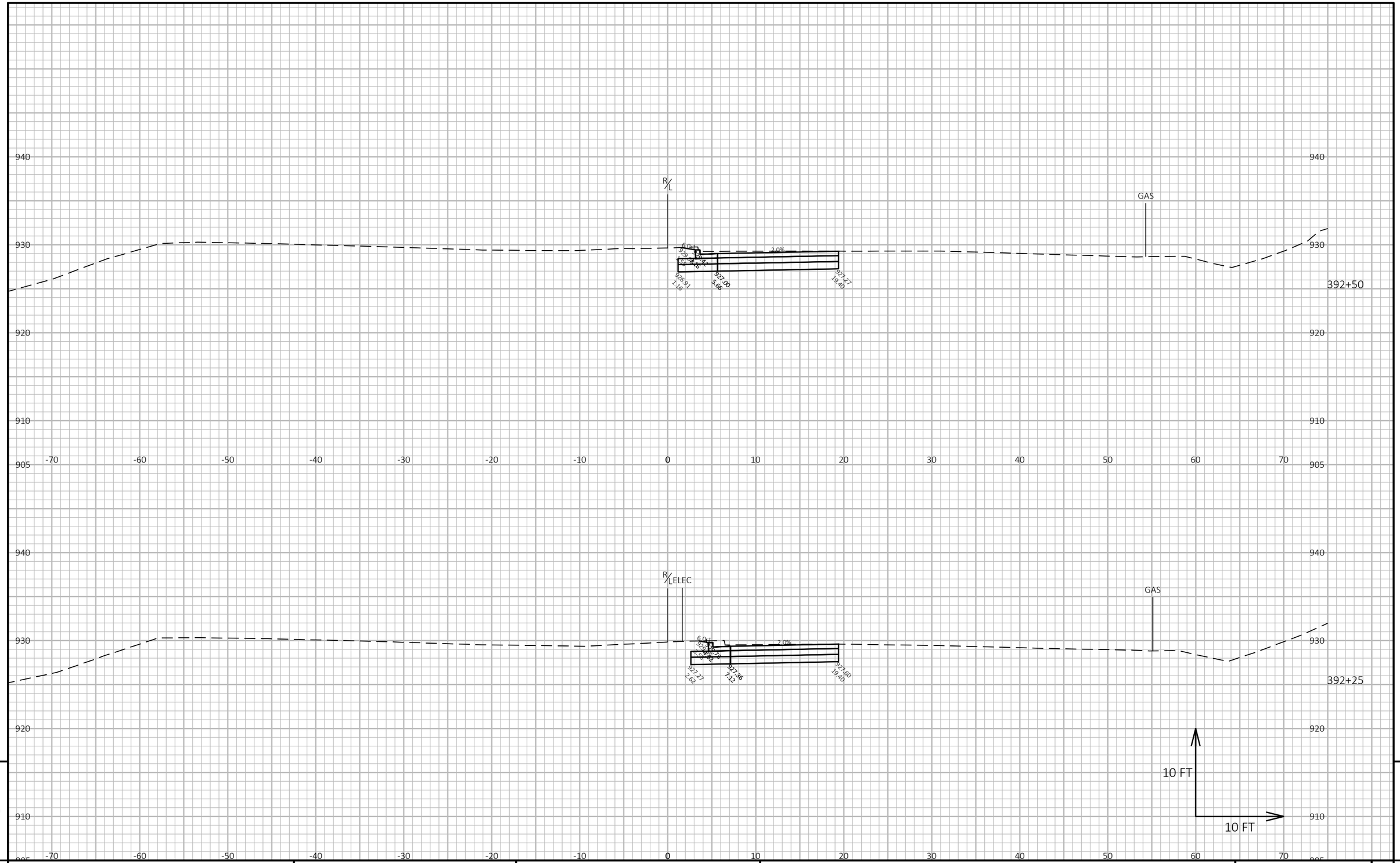
PROJECT NO: 5310-02-78	HWY: USH 14	COUNTY: DANE	CROSS SECTIONS: USH 14	SHEET 87	E

FILE NAME : N:\PDS\C3D\53100208\SHEETSPLAN\090201-XS.DWG
LAYOUT NAME - 11

PLOT DATE : 10/31/2023 8:04 AM PLOT BY : SHAW, ADAM M PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



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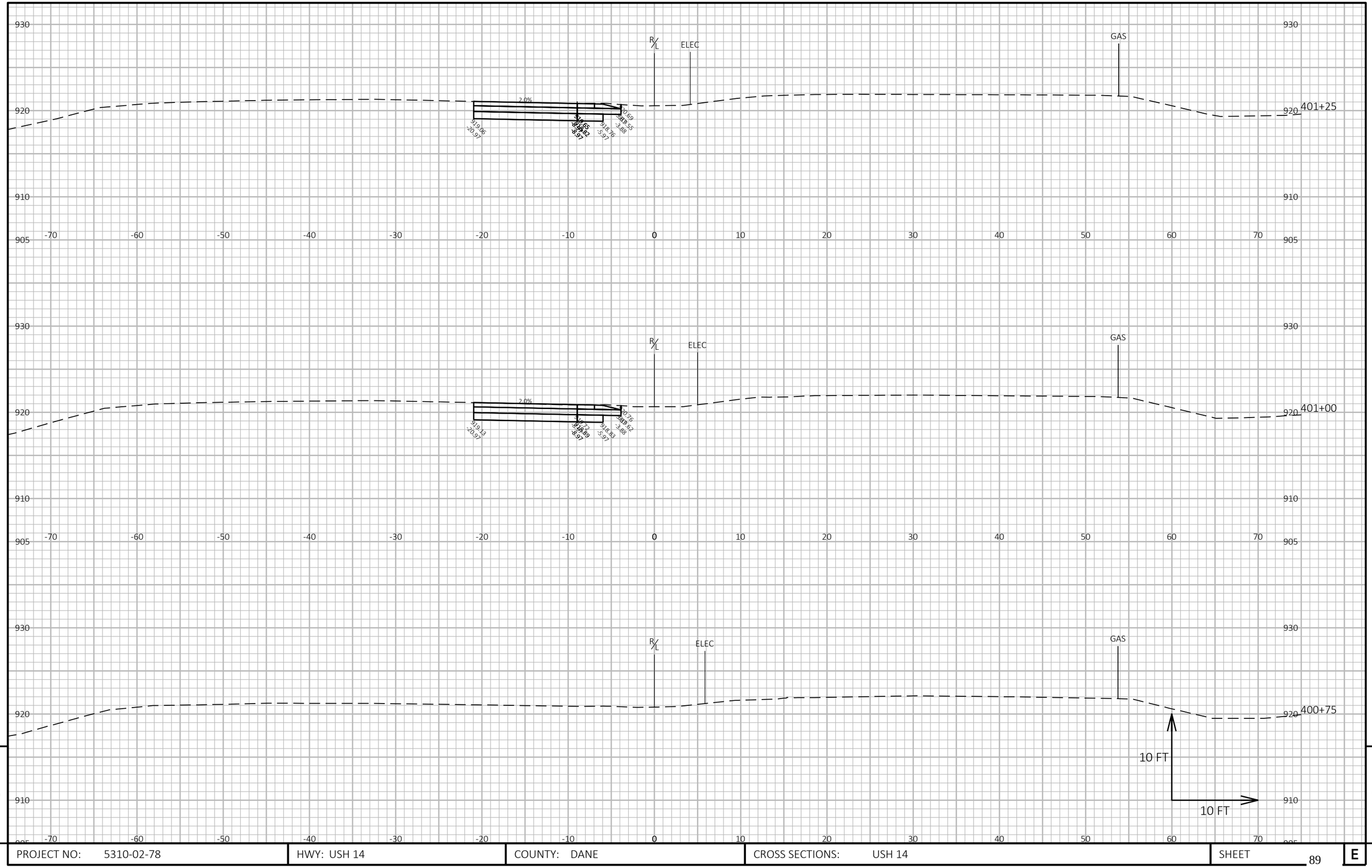
PROJECT NO: 5310-02-78	HWY: USH 14	COUNTY: DANE	CROSS SECTIONS: USH 14	SHEET 88	E

FILE NAME : N:\PDS\C3D\53100208\SHEETSPLAN\090201-XS.DWG
LAYOUT NAME - 12

PLOT DATE : 10/31/2023 8:04 AM PLOT BY : SHAW, ADAM M PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



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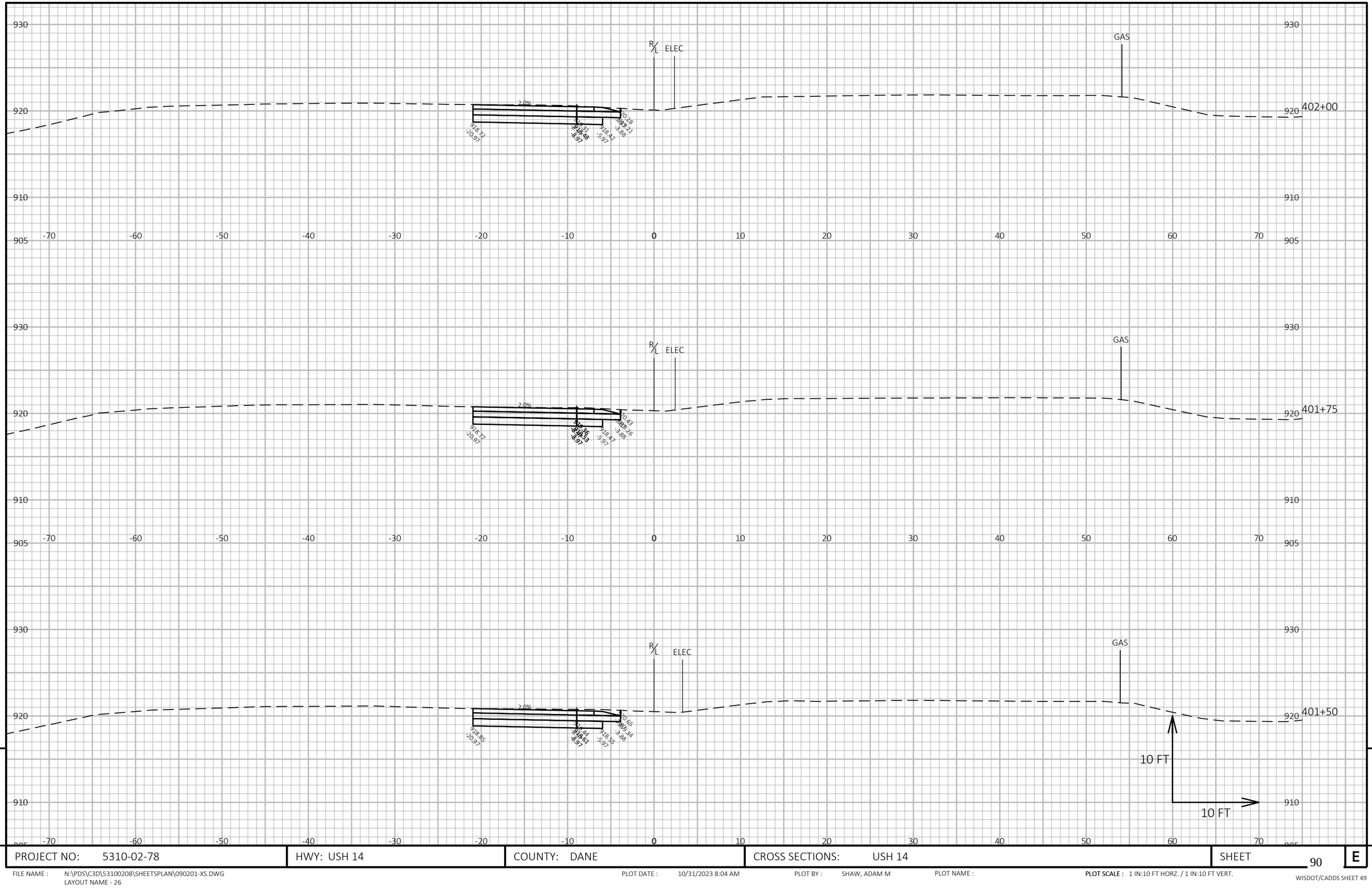
PROJECT NO: 5310-02-78	HWY: USH 14	COUNTY: DANE	CROSS SECTIONS: USH 14	SHEET 89	E

FILE NAME : N:\PDS\C3D\53100208\SHEETSPLAN\090201-XS.DWG
LAYOUT NAME - 25

PLOT DATE : 10/31/2023 8:04 AM PLOT BY : SHAW, ADAM M PLOT NAME : PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



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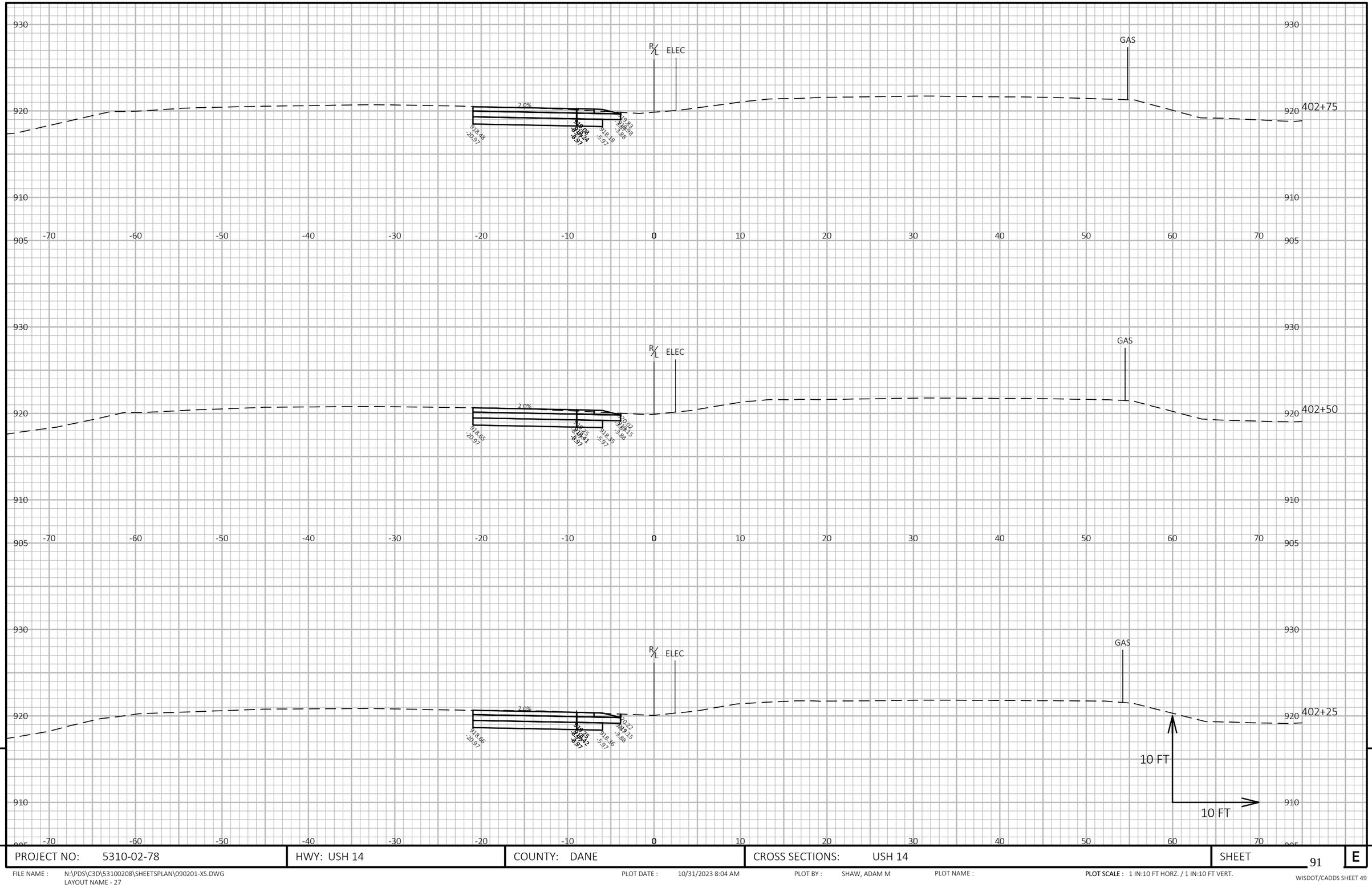
PROJECT NO: 5310-02-78	HWY: USH 14	COUNTY: DANE	CROSS SECTIONS: USH 14	SHEET 90	E

FILE NAME : N:\PDS\C3D\53100208\SHEETSPLAN\090201-XS.DWG
LAYOUT NAME - 26

PLOT DATE : 10/31/2023 8:04 AM PLOT BY : SHAW, ADAM M PLOT NAME : PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.



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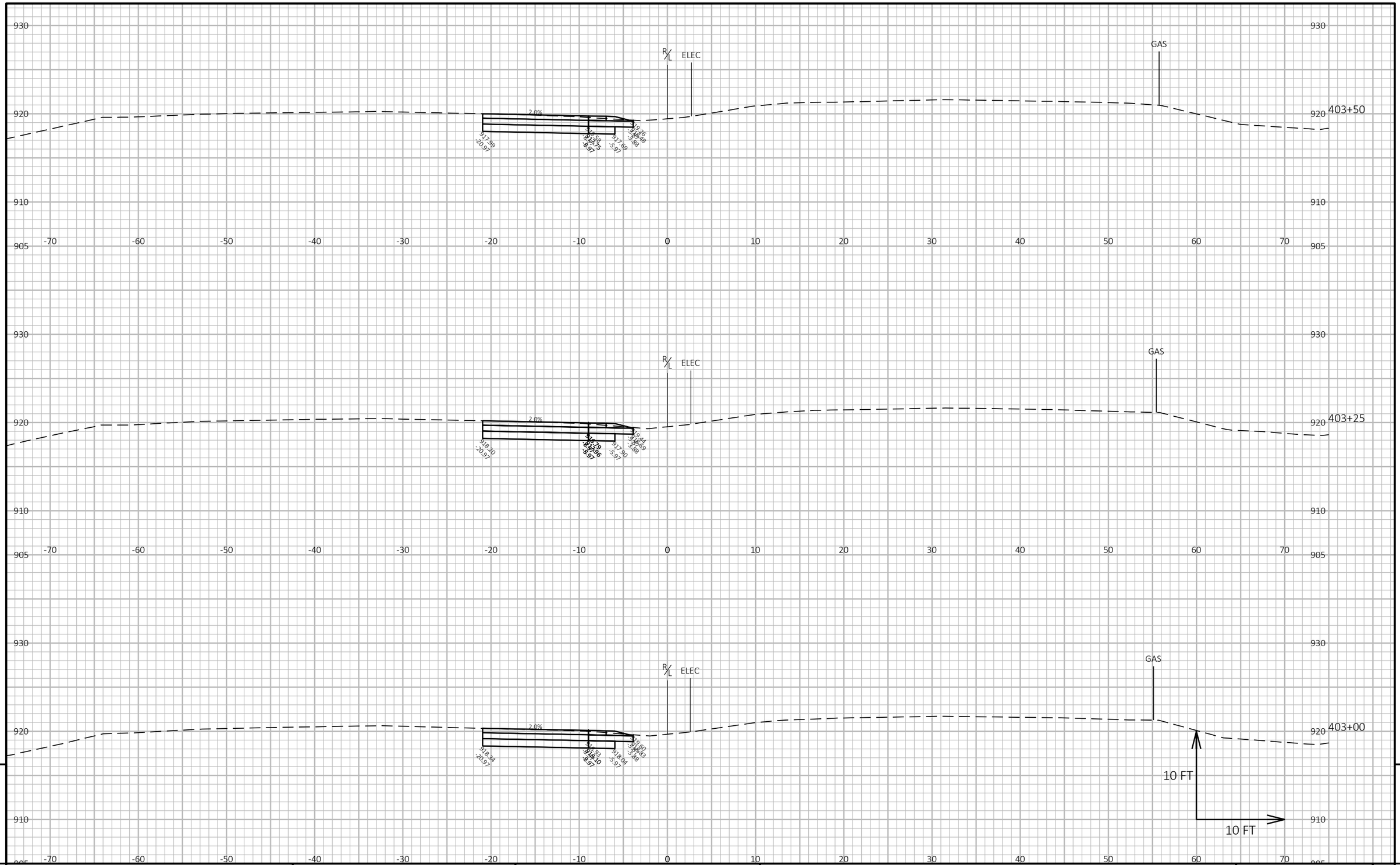
PROJECT NO: 5310-02-78	HWY: USH 14	COUNTY: DANE	CROSS SECTIONS: USH 14	SHEET 91	E

FILE NAME : N:\PDS\C3D\53100208\SHEETSPLAN\090201-XS.DWG
LAYOUT NAME - 27

PLOT DATE : 10/31/2023 8:04 AM PLOT BY : SHAW, ADAM M PLOT NAME : **PLOT SCALE :** 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



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PROJECT NO: 5310-02-78	HWY: USH 14	COUNTY: DANE	CROSS SECTIONS: USH 14	SHEET 92	E

FILE NAME : N:\PDS\C3D\53100208\SHEETSPLAN\090201-XS.DWG
LAYOUT NAME - 28

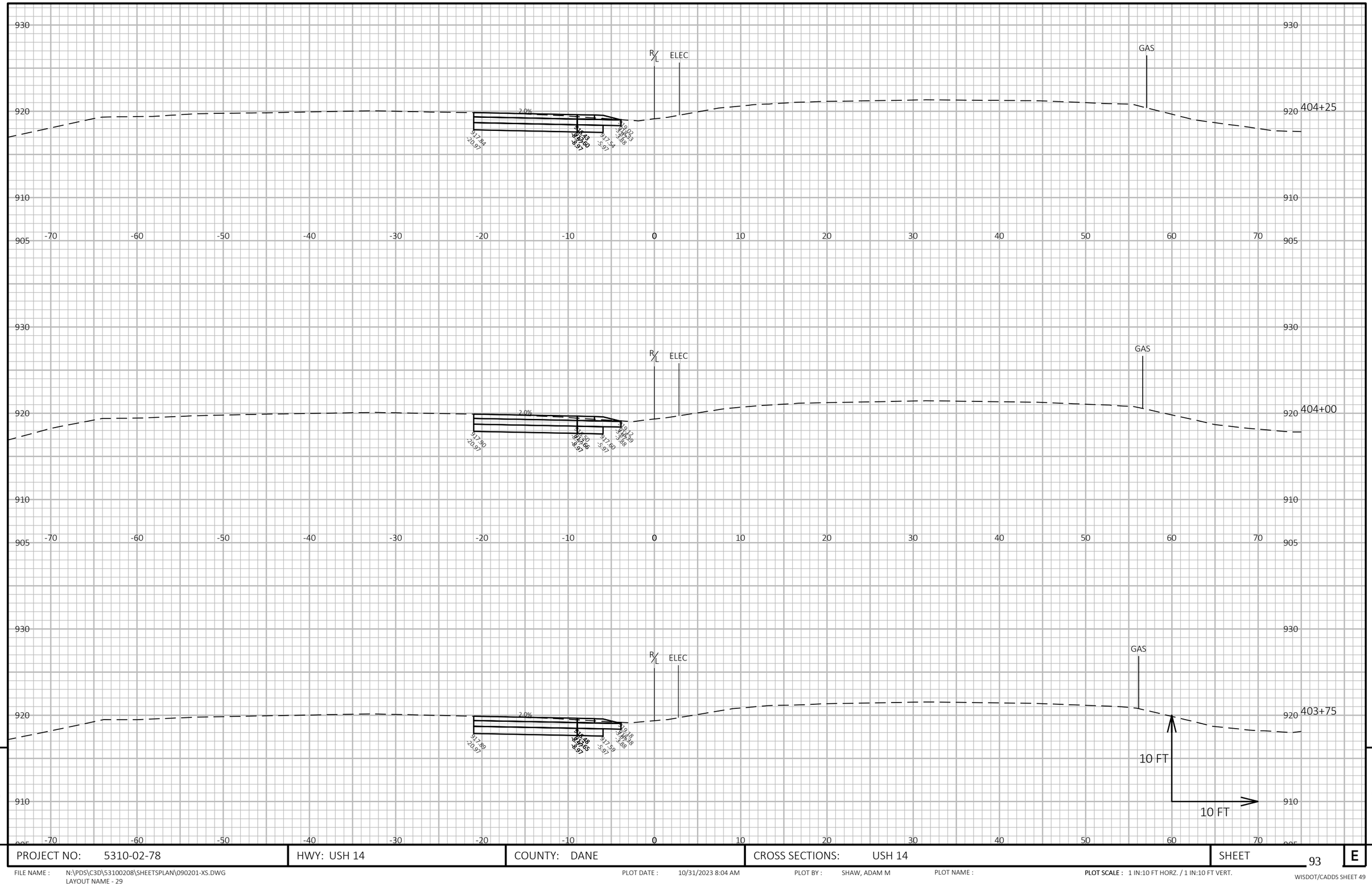
PLOT DATE : 10/31/2023 8:04 AM

PLOT BY : SHAW, ADAM M

PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49

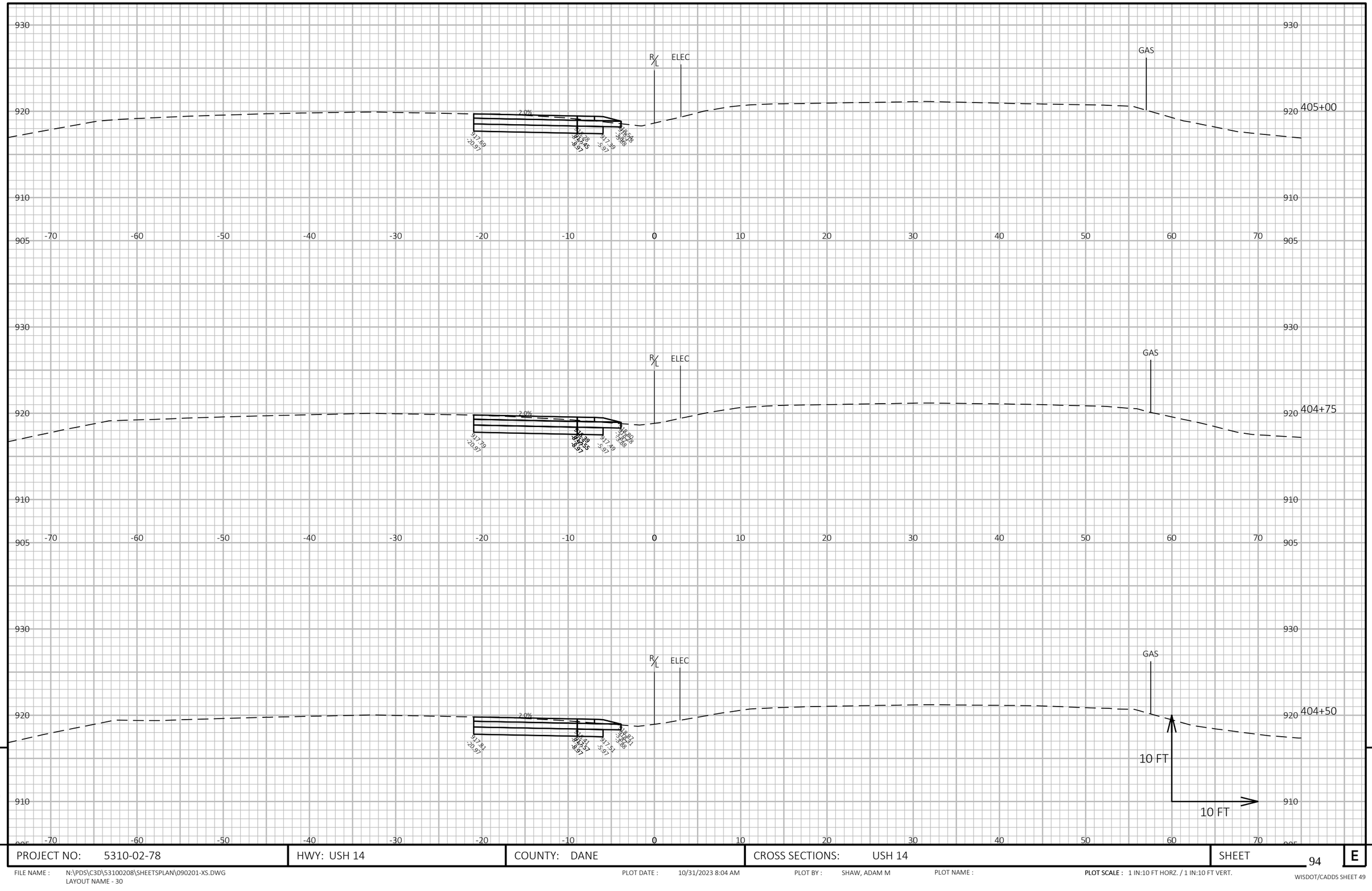


PROJECT NO: 5310-02-78	HWY: USH 14	COUNTY: DANE	CROSS SECTIONS: USH 14	SHEET 93 E

FILE NAME : N:\PDS\C3D\53100208\SHEETSPLAN\090201-XS.DWG
LAYOUT NAME - 29

PLOT DATE : 10/31/2023 8:04 AM PLOT BY : SHAW, ADAM M PLOT NAME : **PLOT SCALE :** 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



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PROJECT NO: 5310-02-78	HWY: USH 14	COUNTY: DANE	CROSS SECTIONS: USH 14	SHEET 94	E

FILE NAME : N:\PDS\C3D\53100208\SHEETSPLAN\090201-XS.DWG
LAYOUT NAME - 30

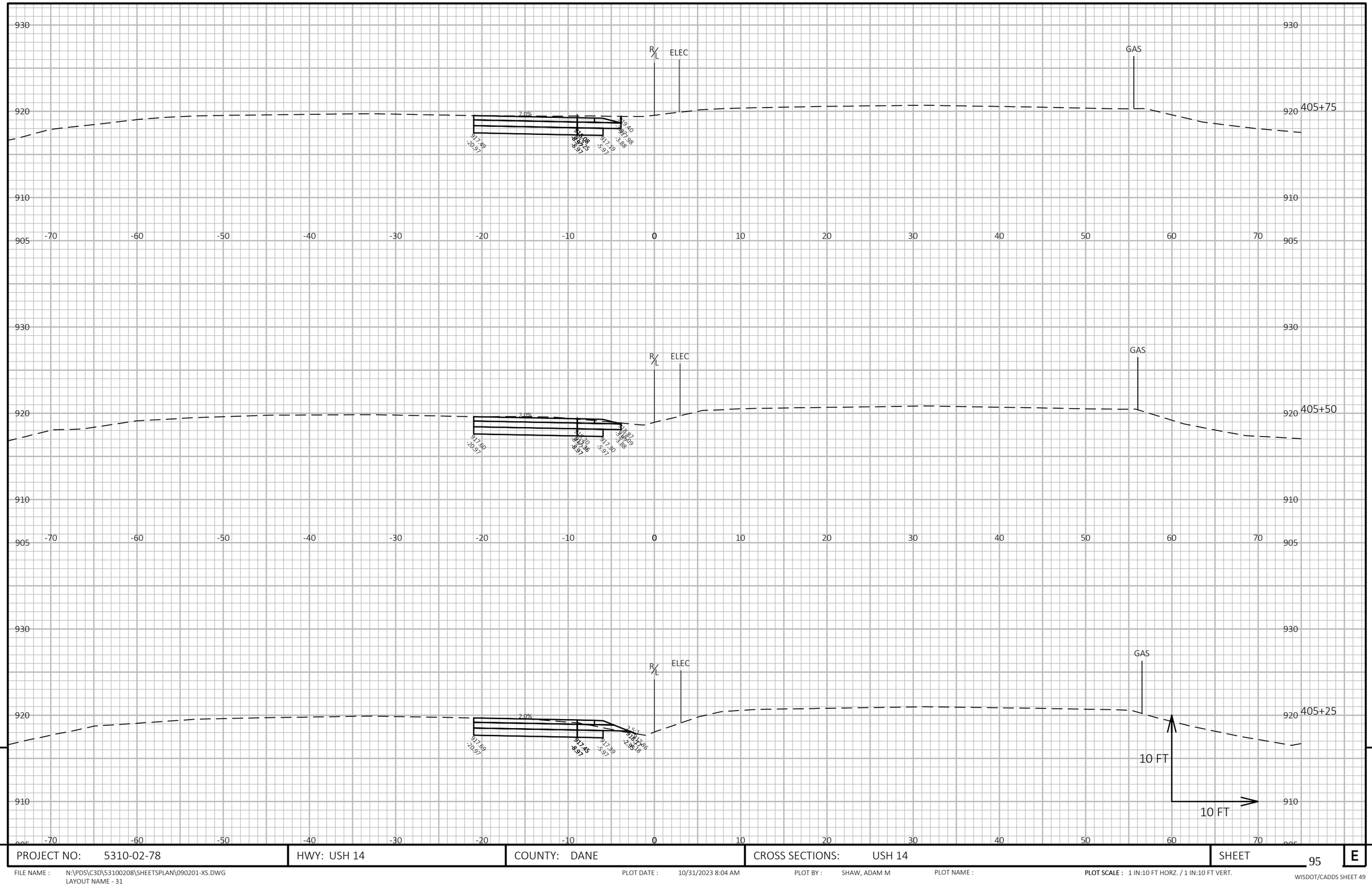
PLOT DATE : 10/31/2023 8:04 AM

PLOT BY : SHAW, ADAM M

PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



9

9

PROJECT NO: 5310-02-78	HWY: USH 14	COUNTY: DANE	CROSS SECTIONS: USH 14	SHEET 95	E

FILE NAME : N:\PDS\C3D\53100208\SHEETSPLAN\090201-XS.DWG
LAYOUT NAME - 31

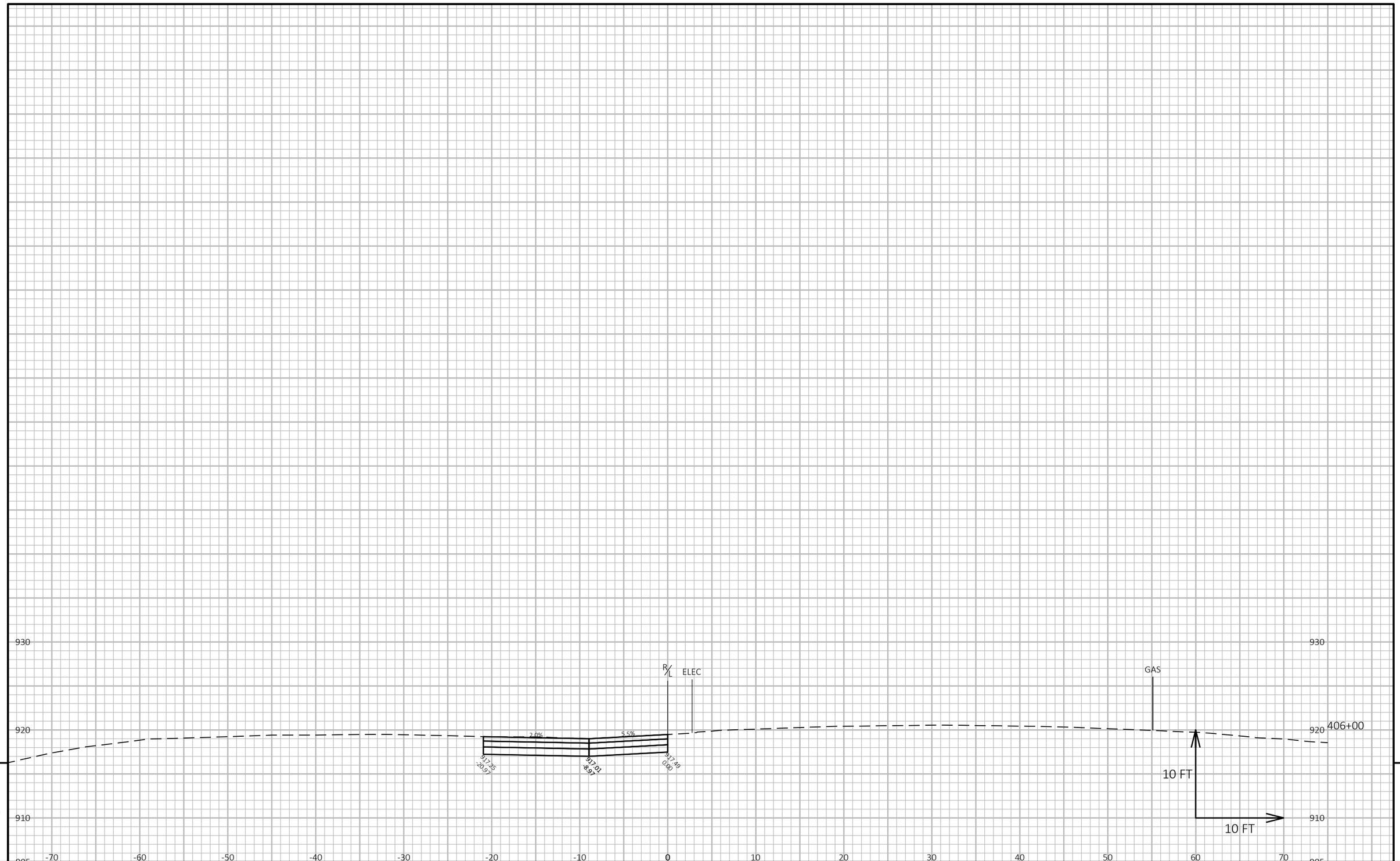
PLOT DATE : 10/31/2023 8:04 AM

PLOT BY : SHAW, ADAM M

PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



9

9

PROJECT NO: 5310-02-78	HWY: USH 14	COUNTY: DANE	CROSS SECTIONS: USH 14	SHEET 96	E

FILE NAME : N:\PDS\C3D\53100208\SHEETSPLAN\090201-XS.DWG
LAYOUT NAME - 32

PLOT DATE : 10/31/2023 8:04 AM PLOT BY : SHAW, ADAM M PLOT NAME : **PLOT SCALE :** 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

Notes

Wisconsin Department of Transportation

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