WKE

MARCH 2024

PROJECT ID: 1320-07-73

COUNTY: RACINE

ORDER OF SHEETS

TOTAL SHEETS = 204

09

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

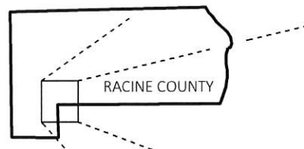
PLAN OF PROPOSED IMPROVEMENT

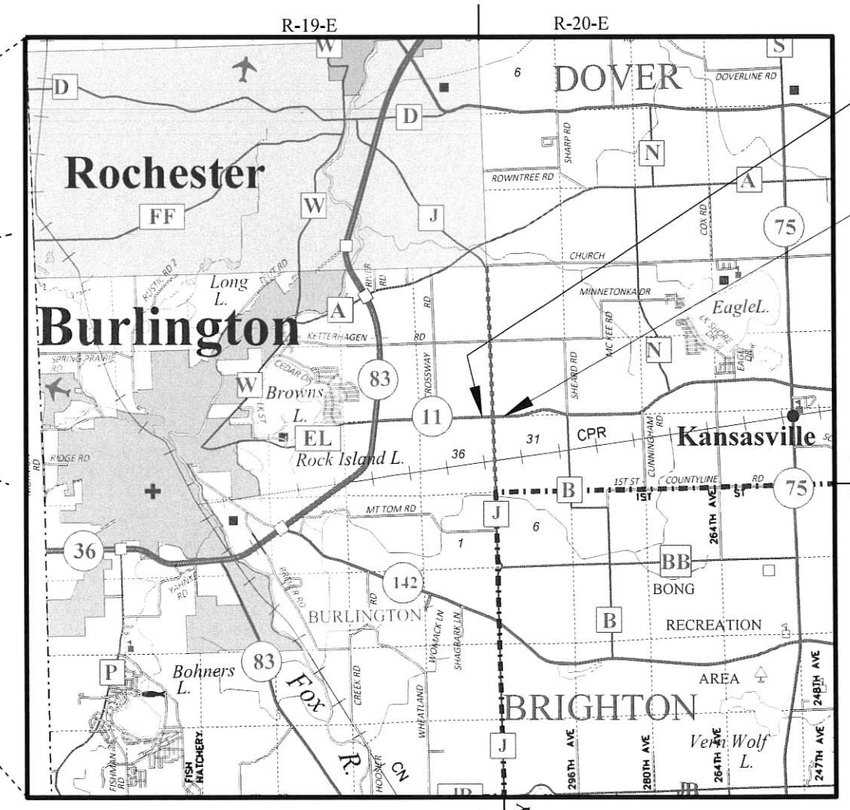
BURLINGTON - RACINE
CTH J

STH 11
RACINE COUNTY

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
1320-07-73	WISC 2024275	1

STATE PROJECT NUMBER
1320-07-73





BEGIN PROJECT
STA 205+11.40'EB'
X=530296.373
Y=170366.060

END PROJECT
STA 217+76.03'EB'
X=531551.424
Y=170373.403

T-3-N RACINE COUNTY
T-2-N KENOSHA COUNTY

DESIGN DESIGNATION

A.A.D.T.	2017	=	6,800/1,300
A.A.D.T.	2045	=	7,600/1,600
D.H.V.		=	15.0
D.D.		=	60/40
T.		=	15.1%
DESIGN SPEED		=	60 MPH (BOTH STH 11 AND CTH J)
ESALS		=	394 (DESIGN LANE DAILY)

CONVENTIONAL SYMBOLS

PLAN		PROFILE	
CORPORATE LIMITS		GRADE LINE	
PROPERTY LINE		ORIGINAL GROUND	
LOT LINE		MARSH OR ROCK PROFILE (To be noted as such)	ROCK
LIMITED HIGHWAY EASEMENT		SPECIAL DITCH	LABEL
EXISTING RIGHT OF WAY		GRADE ELEVATION	95.36
PROPOSED OR NEW R/W LINE		CULVERT (Profile View)	
SLOPE INTERCEPT		UTILITIES	
REFERENCE LINE	300'EB'	ELECTRIC	E
EXISTING CULVERT		FIBER OPTIC	FO
PROPOSED CULVERT (Box or Pipe)		GAS	G
COMBUSTIBLE FLUIDS	CAUTION	SANITARY SEWER	SAN
		STORM SEWER	SS
		TELEPHONE	T
MARSH AREA		WATER	W
		UTILITY PEDESTAL	
		POWER POLE	
WOODED OR SHRUB AREA		TELEPHONE POLE	

LAYOUT
SCALE 0 2 MI

TOTAL NET LENGTH OF CENTERLINE = 0.240 MI

RACINE COUNTY
KENOSHA COUNTY

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COUNTY COORDINATE SYSTEM (WCCS), RACINE COUNTY NAD83 (2011), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES.

ELEVATIONS ARE REFERENCED TO NAVD 88 (2012).

ORIGINAL PLANS PREPARED BY
AYRES

WISCONSIN
KEVIN M. KUHLOW
E-34276
MADISON
WI
PROFESSIONAL ENGINEER

DATE: 10/31/2023 (Professional Engineer Signature)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY

Surveyor	AYRES ASSOCIATES
Designer	AYRES ASSOCIATES
Project Manager	NGUYEN LY
Regional Examiner	STEVE CHOJNACKI
Regional Supervisor	BENEDICT ERUCHALU

APPROVED FOR THE DEPARTMENT

DATE: 10/31/2023 Nguyen K. LY (Signature)

E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\010101-TI.DWG PLOT DATE : 10/17/2023 9:20 AM PLOT BY : KUSCHEL, LEVI PLOT NAME :

GENERAL NOTES

1. NO TREES OR SHRUBS SHALL BE REMOVED WITHOUT APPROVAL OF THE ENGINEER.
2. THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLAN ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.
3. THE CONTRACTOR SHALL COORDINATE THEIR CONSTRUCTION ACTIVITIES WITH A CALL TO DIGGERS HOTLINE AND/OR A DIRECT CALL TO THE UTILITIES THAT HAVE FACILITIES IN T HE AREA. NOT ALL UTILITIES ARE MEMBERS OF DIGGERS HOTLINE.
4. A SAW JOINT IS REQUIRED WHERE NEW HMA PAVEMENT MEETS EXISTING HMA PAVEMENT.
5. SAWCUT ASPHALT AT THE MATCHLINE AS SHOWN ON THE PLAN DETAILS OR AS DIRECTED BY THE ENGINEER.
6. PRIOR TO ORDERING DRAINAGE PIPES AND STRUCTURES, VERIFY RELATED DRAINAGE INFORMATION IN THE PLAN AND PROVIDE DOCUMENTATION TO THE ENGINEER.
7. WETLANDS, WATERWAYS, AND OTHER ENVIRONMENTALLY SENSITIVE AREAS SHALL BE PROTECTED AT ALL TIMES. DO NOT STORE EQUIPMENT OR MATERIAL NEAR THESE SITES UNLESS APPROVED BY THE ENGINEER.
8. PLACE EROSION CONTROL DEVICES IN SEQUENCE WITH CONSTRUCTION OPERATIONS OR AS DETERMINED BY THE ENGINEER. EROSION CONTROL FEATURES ARE SHOWN AT APPROXIMATE LOCATIONS, WITH EXACT LOCATIONS TO BE DETERMINED BY THE ENGINEER. REMOVAL OF ITEMS ARE INCIDENTAL TO THE RESPECTIVE EROSION CONTROL BID ITEM COSTS.
9. DO NOT USE FERTILIZER WITHIN 20 FEET OF NAVIGABLE WATERWAYS OR WETLANDS.
10. PLACE SALVAGED TOPSOIL IN ALL GRADED AREAS AS DESIGNATED BY THE ENGINEER IMMEDIATELY AFTER GRADING HAS BEEN COMPLETED. PLACE SEED, EROSION MAT, AND FERTILIZE ALL AREAS 5 DAYS AFTER PLACEMENT OF SALVAGED TOPSOIL.
11. STATIONING, DISTANCES, AND OFFSETS FOR SIGNS AND TRAFFIC CONTROL DEVICES SHOWN IN THE PLANS ARE APPROXIMATE. EXACT LOCATIONS ARE DETERMINED BY THE ENGINEER.
12. COVER ALL SIGNS IN CONFLICT WITH TRAFFIC CONTROL "IN USE" AS SHOWN IN THE PLANS OR AS DIRECTED BY THE ENGINEER.
13. RESHAPING AND RESTORING OF ANY PREVIOUSLY GRASSED AREAS WHICH ARE DISTURBED BY OPERATIONS OUTSIDE OF THE ENGINEER DETERMINED CONSTRUCTION LIMITS ARE INCIDENTAL TO THE CONTRACT.
14. CONTACT THE PROJECT ENGINEER AND THE SOUTHEASTERN WISCONSIN REGIONAL PLANNING COMMISSION, AT LEAST TWO WEEKS PRIOR TO WORK NEAR ANY PUBLIC SURVEY MONUMENT.
15. HMA PAVEMENTS:

ROADWAY (INCLUDING SHOULDERS ABUTTING CONCRETE CURB AND GUTTER)

7¾-INCH = 3-INCH LOWER LAYER (3 MT 58-28 S)
3-INCH MIDDLE LAYER (3 MT 58-28 S)
1¾-INCH UPPER LAYER (4 MT 58-28 H)

SHOULDERS (PAVED INTEGRAL WITH UPPER TWO LAYERS OF TRAVEL LANES)

4¾-INCH = 3-INCH LOWER LAYER (3 MT 58-28 S)
1¾-INCH UPPER LAYER (4 MT 58-28 H)

STANDARD ABREVIATIONS

A.D.T.	AVERAGE DAILY TRAFFIC
AECP	APRON ENDWALL FOR CULVERT PIPE
AECPRC	APRON ENDWALL FOR CULVERT PIPE REINFORCED CONCRETE
AECPS	APRON ENDWALL FOR CULVERT PIPE STEEL
AECPSAL	APRON ENDWALL FOR CULVERT PIPE SALVAGED
ATMS	ARTERIAL TRAFFIC MANAGEMENT SYSTEM
BM	BENCHMARK
BOC	BACK OF CURB
CBTP	CONCRETE BARRIER TEMPORARY PRECAST
CBSS	CONCRETE BARRIER SINGLE SLOPE
CL	CLASS
CONST	CONSTRUCTION
CP	CONTROL POINT
CPCS	CULVERT PIPE CORRUGATED STEEL
CPRC	CULVERT PIPE REINFORCED CONCRETE
D.D.	DIRECTIONAL DISTRIBUTION
D.H.V.	DESIGN HOURLY VOLUME
EB	EASTBOUND
EXIST	EXISTING
HMA	HOT MIX ASPHALT
H.S.	HIGH STRENGTH
ITS	INTELLIGENT TRAFFIC SYSTEM
MAX	MAXIMUM
MIN	MINIMUM
NB	NORTHBOUND
NOR	NORMAL
PC	POINT OF CURVATURE
PCC	POINT OF COMMON CURVATURE
PI	POINT OF INTERSECTION
PRC	POINT OF REVERSE CURVATURE
PT	POINT OF TANGENCY
R/L	REFERENCE LINE
REQ'D	REQUIRED
SB	SOUTHBOUND
SSPRC	STORM SEWER PIPE REINFORCED CONCRETE
T.	PERCENT TRUCKS
TYP	TYPICAL
VAR	VARIABLE
WB	WESTBOUND
Wt.	WEIGHT

UTILITY CONTACTS

AT&T WISCONSIN - COMMUNICATION LINE
MICHAEL VANBOVEN
411 7TH ST
RACINE, WI 53406
PHONE: (262) 676-3958
EMAIL: MV3658@ATT.COM

SPECTRUM - COMMUNICATION LINE
JAY CHAPMAN
1320 N DR MARTIN LUTHER KING JR DR
MILWAUKEE, WI 53212
PHONE: (414) 908-4793
CELL: (414) 639-5296
EMAIL: JAY.CHAPMAN@CHARTER.COM

SOUTHEAST TELEPHONE COMPANY OF WISCONSIN, LLC DBA TDS TELECOM - COMMUNICATION LINE
DAVE HUWE
525 JUNCTION RD
MADISON, WI 53717
PHONE: (608) 664-4312
EMAIL: DAVE.HUWE@TDSTELECOM.COM

WE ENERGIES - ELECTRICITY
ADAM PSICIHULIS
500 S 116TH STREET
WEST ALLIS, WI 53214
PHONE: (262) 763-1011
MOBILE: (262) 490-1259
EMAIL: ADAM.PSICIHULIS@WE-ENERGIES.COM

WE ENERGIES - GAS
SCOTT HOLSTEIN
700 S KANE STREET
BURLINGTON,WI 53105
PHONE: (262) 763-1048
MOBILE: (262) 949-0490
EMAIL: SCOTT.HOLSTEIN@WE-ENERGIES.COM

WINDSTREAM (KDL) - COMMUNICATION LINE
LORI KETTER
314 N DANZ AVE
GREEN BAY, WI 54302-3562
PHONE: (920) 410-6902
EMAIL: LORI.KETTER@WINDSTREAM.COM

* DENOTES NOT A MEMBER OF DIGGERS HOTLINE

DIGGERS HOTLINE
Dial 811 or (800)242-8511
www.DiggersHotline.com

ORDER OF SECTION 2 SHEETS

PROJECT CONTACTS

WisDOT PROJECT MANAGER

NGUYEN LY
141 NW BARSTOW STREET
WAUKESHA, WI 53187-0798
PHONE: (262) 548-8739
EMAIL: NGUYEN.LY@DOT.WI.GOV

DNR AREA LIASON

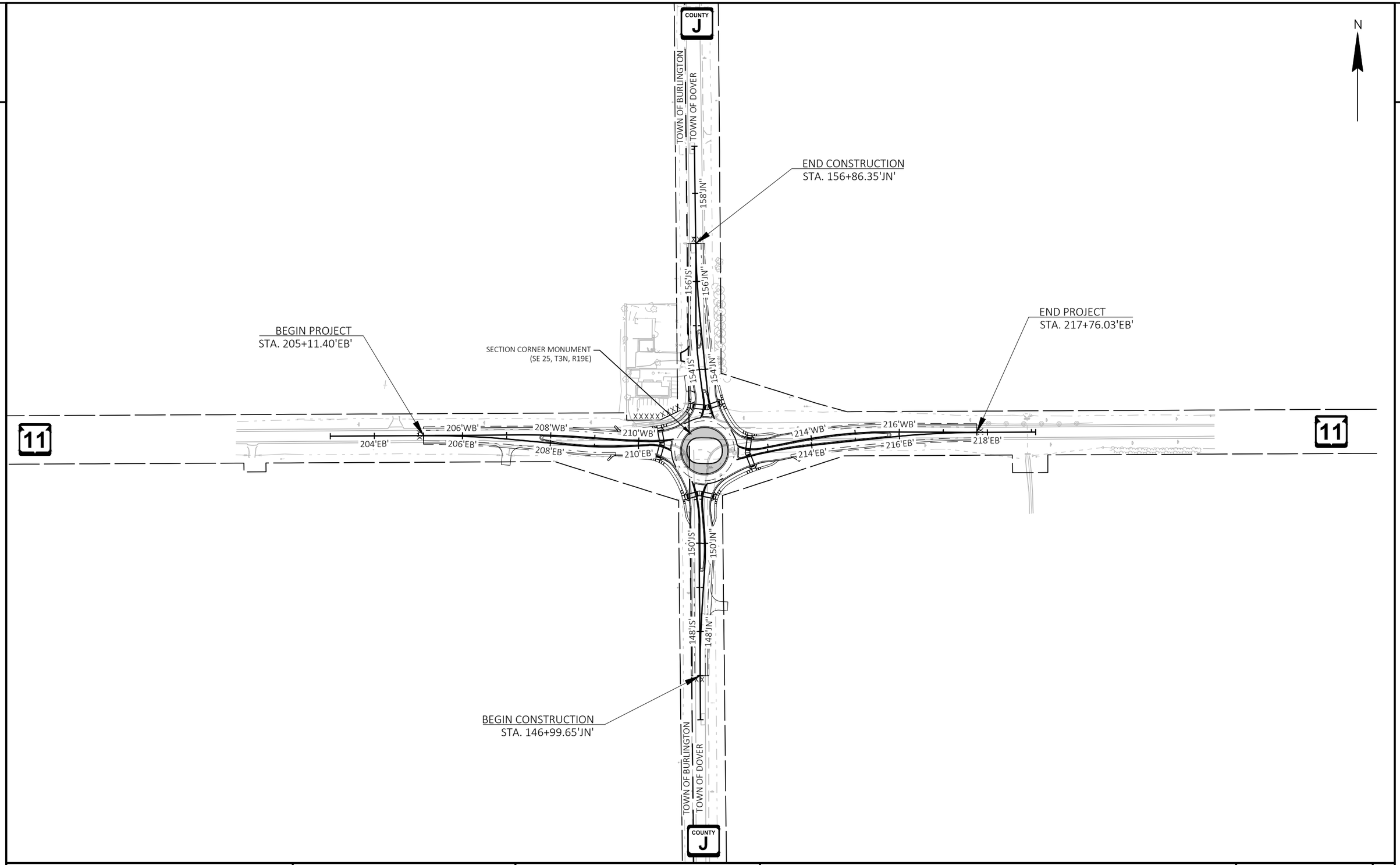
BENTON STETZEL
WISCONSIN DEPARTMENT OF NATURAL RESOURCES
141 NW BARSTOW ST, #180
WAUKESHA, WI 53188
PHONE: (262) 623-0194
EMAIL: BENTON.STETZEL@WISCONSIN.GOV

CONSULTANT DESIGN

KEVIN KUHLOW
AYRES ASSOCIATES
5201 E. TERRACE DRIVE, SUITE 200
MADISON, WI 53718
PHONE: (608) 443-1210
EMAIL: KUHLOWK@AYRESASSOCIATES.COM

SEWRPC

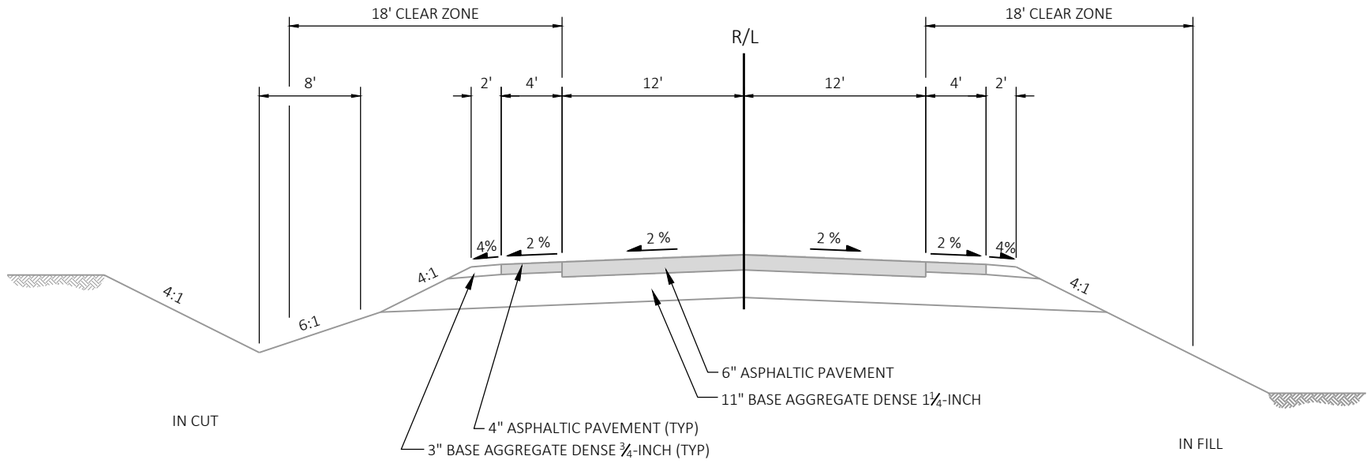
SOUTHEASTERN WISCONSIN REGIONAL PLANNING COMMISSION
ROB MERRY
W239 N1812 ROCKWOOD DR
P.O. BOX 1607
WAUKESHA, WI 53187
PHONE: (262) 953-4289
EMAIL: RMERRY@SEWRPC.ORG



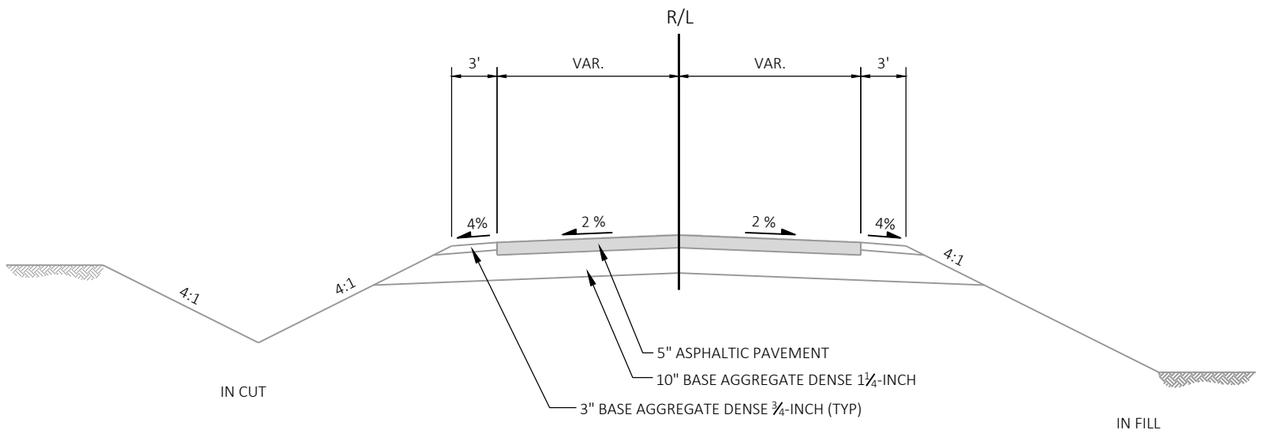
PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	PROJECT OVERVIEW	SHEET 3	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\020201-PO.DWG
LAYOUT NAME - 01
PLOT DATE : 6/14/2023 7:32 AM
PLOT BY : MUENCH, DOUGLAS
PLOT NAME :
PLOT SCALE : 1 IN:200 FT
WISDOT/CADDS SHEET 42

2

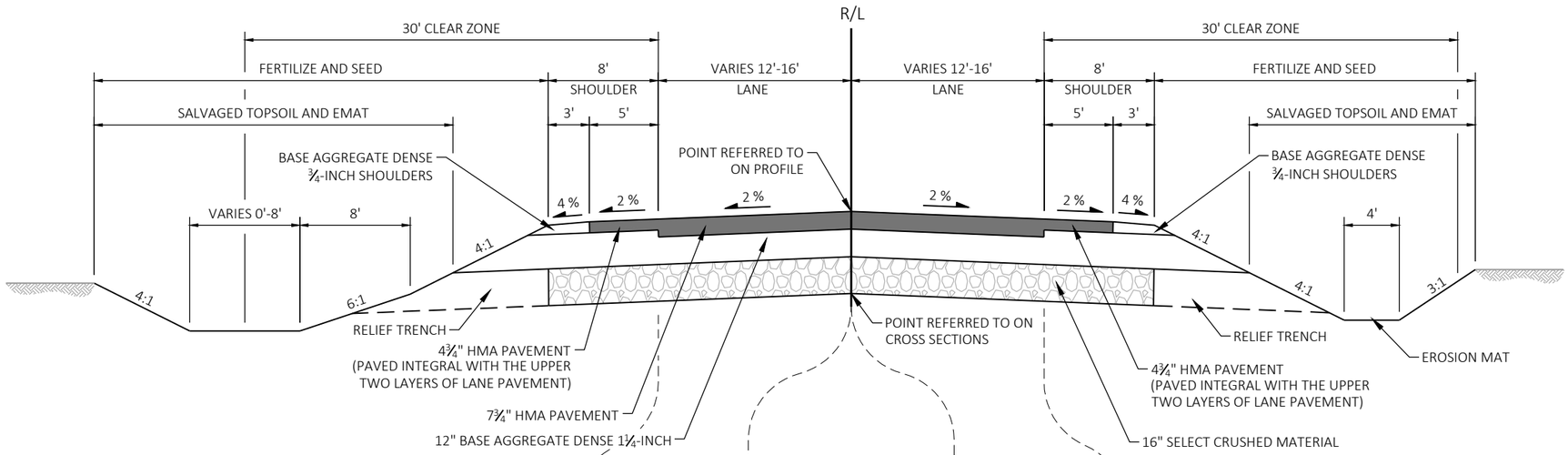


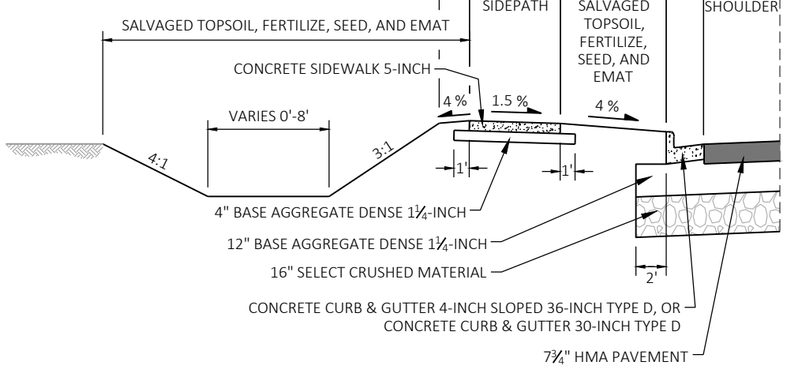
TYPICAL EXISTING SECTION - STH 11



TYPICAL EXISTING SECTION - CTH J

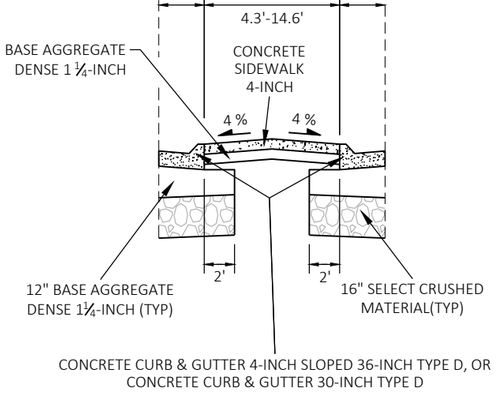
PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	TYPICAL SECTIONS	SHEET 4	E





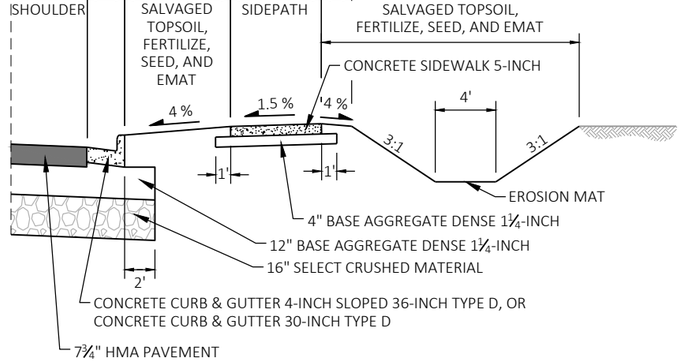
CURB & GUTTER AND PATH , LT

STA 209+65'EB' TO STA 210+75'EB' (30-INCH C&G)
STA 213+03'EB' TO STA 213+60'EB' (36-INCH C&G)
STA 150+18'JN' TO STA 151+34'JN' (36-INCH C&G)
STA 152+91'JN' TO STA 152+02'JN' (30-INCH C&G)



MEDIAN

STA 207+75'EB' TO STA 209+85'EB' (36-INCH C&G)
STA 209+85'EB' TO STA 210+75'EB' (30-INCH C&G)
STA 212+30'EB' TO STA 213+27'EB' (30-INCH C&G)
STA 213+27'EB' TO STA 215+81'EB' (36-INCH C&G)
STA 149+36'JN' TO STA 150+45'JN' (36-INCH C&G)
STA 150+45'JN' TO STA 151+34'JN' (30-INCH C&G)
STA 152+91'JN' TO STA 153+55'JN' (30-INCH C&G)
STA 154+49'JN' TO STA 154+90'JN' (30-INCH C&G)



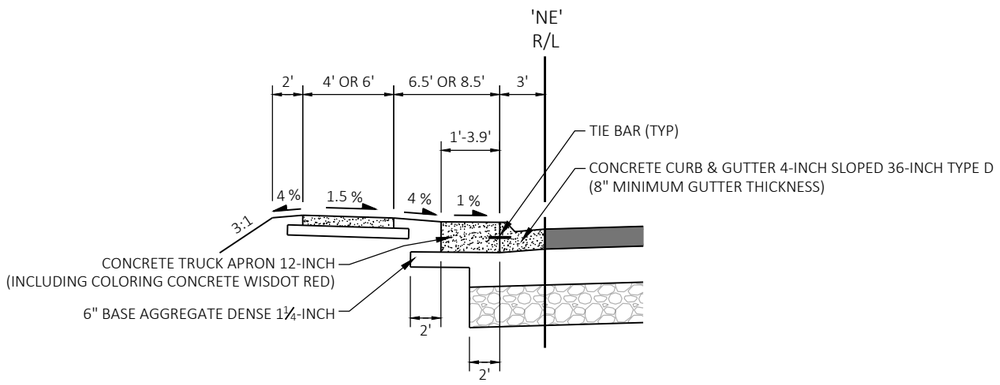
CURB & GUTTER AND PATH, RT

STA 209+47'EB' TO STA 210+11'EB' (36-INCH C&G)
STA 212+30'EB' TO STA 213+50'EB' (30-INCH C&G)
STA 150+08'JN' TO STA 151+34'JN' (30-INCH C&G)
STA 152+91'JN' TO STA 154+05'JN' (36-INCH C&G)

TYPICAL FINISHED SECTION - STH 11/CTH J

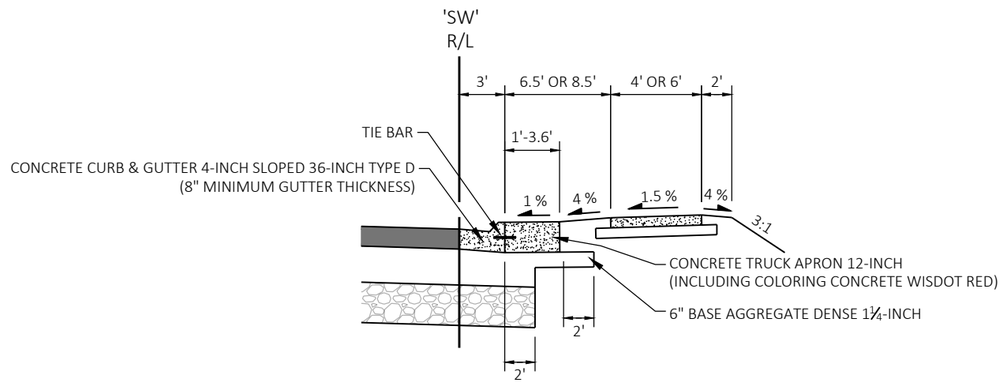
STA 205+11.40'EB' TO STA 210+75'EB'
STA 212+30'EB' TO STA 217+76.03'EB'
STA 146+99.65'JN' TO STA 151+34'JN'
STA 152+90'JN' TO STA 156+86.35'JN'

PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	TYPICAL SECTIONS	SHEET 5	E



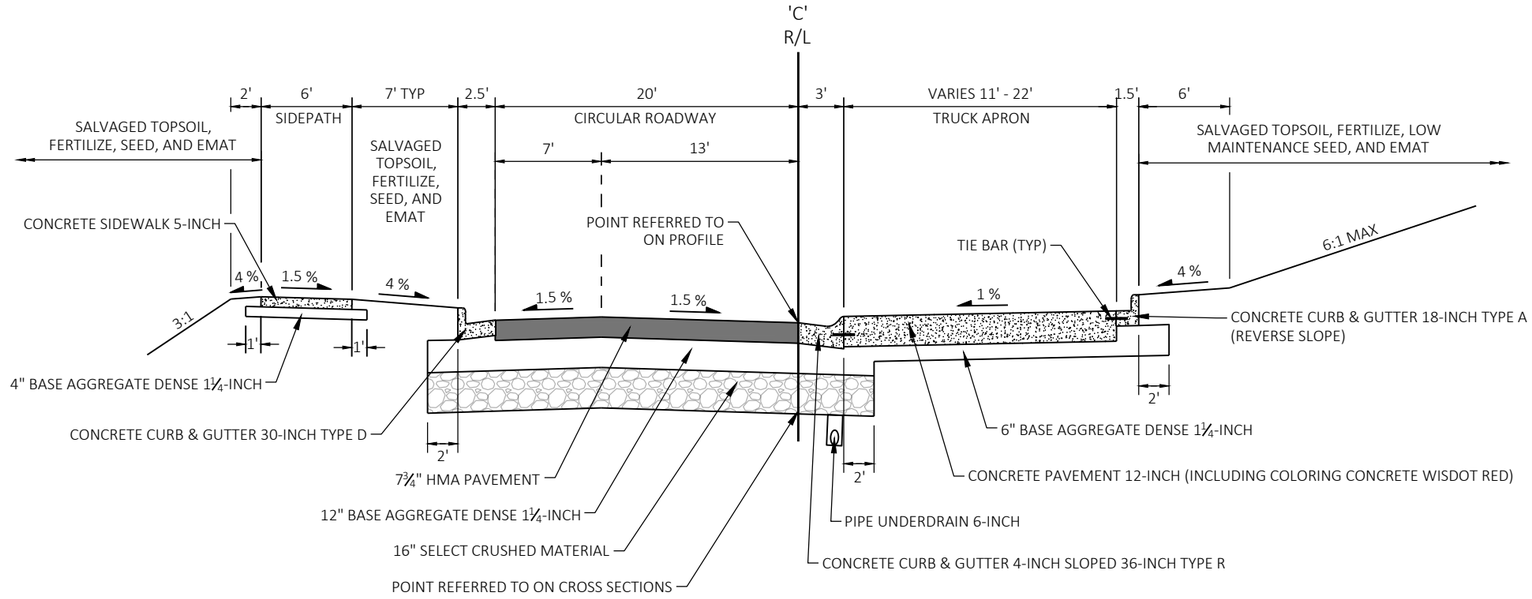
TYPICAL FINISHED SECTION - TRUCK APRON 'WB'

STA 212+39.66'NE' TO STA 213+00.45'NE'



TYPICAL FINISHED SECTION - TRUCK APRON 'EB'

STA 210+11.46'SW' TO STA 210+72.95'SW'



TYPICAL FINISHED HALF-SECTION - ROUNDABOUT

PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	TYPICAL SECTIONS	SHEET 6	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\020301-TS.DWG
LAYOUT NAME - FTYP_02

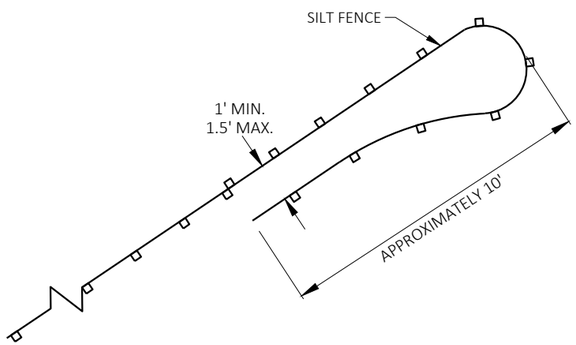
PLOT DATE : 12/18/2023 2:49 PM PLOT BY : KUSCHEL, LEVI PLOT NAME : PLOT SCALE : 1 IN:10 FT

WISDOT/CADDS SHEET 42

A	HYDROLOGIC SOIL GROUP											
	B		C						D			
	SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)		
LAND USE:	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER
ROW CROPS	.08 .22	.16 .30	.22 .38	.12 .26	.20 .34	.27 .44	.15 .30	.24 .37	.33 .50	.19 .34	.28 .41	.38 .56
MEDIAN STRIP-TURF	.19 .24	.20 .26	.24 .30	.19 .25	.22 .28	.26 .33	.20 .26	.23 .30	.30 .37	.20 .27	.25 .32	.30 .40
SIDE SLOPE: TURF			.25 .32			.27 .34			.28 .36			.30 .38
PAVEMENT:												
ASPHALT	.70 - .95											
CONCRETE	.80 - .95											
BRICK	.70 - .80											
DRIVES, WALKS	.75 - .85											
ROOFS	.75 - .95											
GRAVEL ROADS, SHOULDERS	.40 - .60											

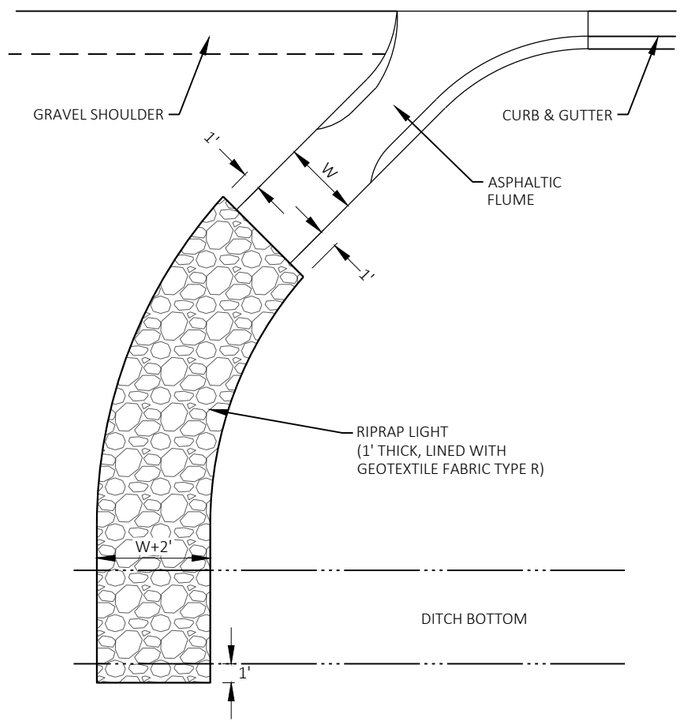
TOTAL PROJECT AREA = 5.79 ACRES
TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = 6.71 ACRES

RUNOFF COEFFICIENT TABLE

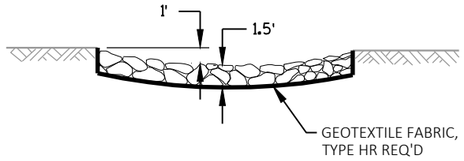


SILT FENCE TURNAROUND END DETAIL

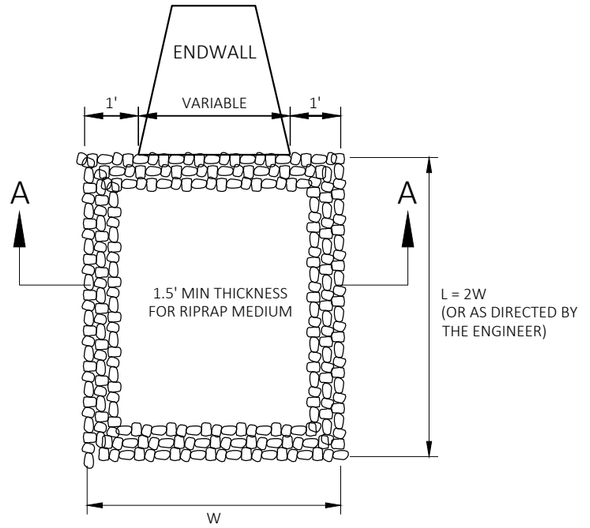
NOTES:
1. TURNAROUNDS INTENDED TO REDIRECT AMPHIBIANS AND REPTILES AWAY FROM THE CONSTRUCTION ZONE.
2. SILT FENCE POSTS FOR THE TURN-AROUND SHOULD BE ON THE OUTSIDE OF THE TURN-AROUND.
3. SEE EROSION CONTROL PLAN FOR LOCATIONS.



DETAIL FOR RIPRAP LIGHT AT ASPHALTIC FLUMES



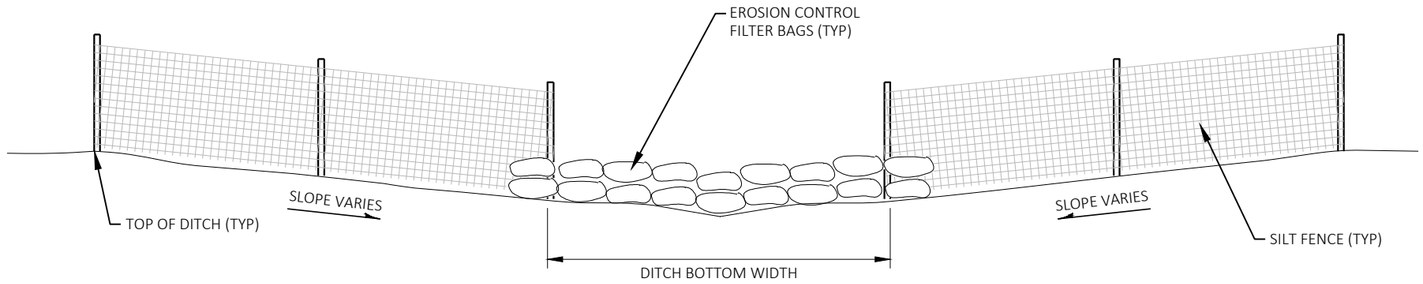
SECTION A-A



DETAIL FOR RIPRAP MEDIUM TREATMENT AT CULVERTS

2

SECTION A-A



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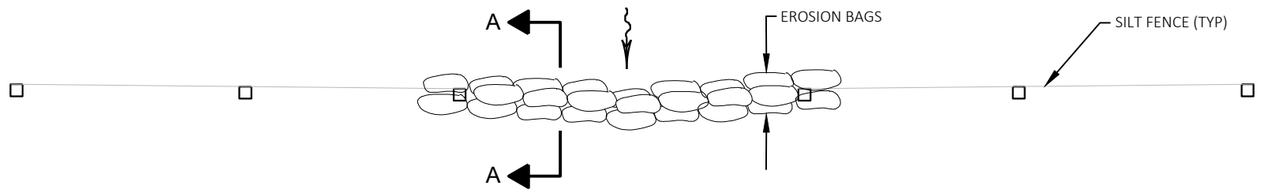
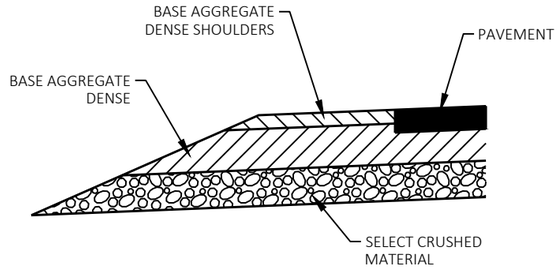


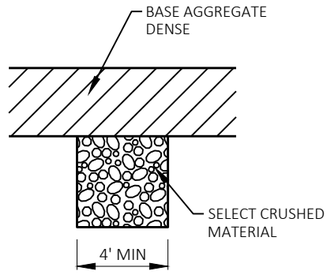
PLAN

SILT FENCE RELIEF DETAIL

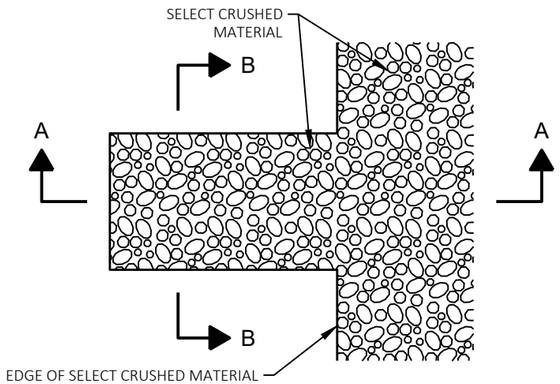
PAID FOR AS ROCK BAGS



SECTION A-A

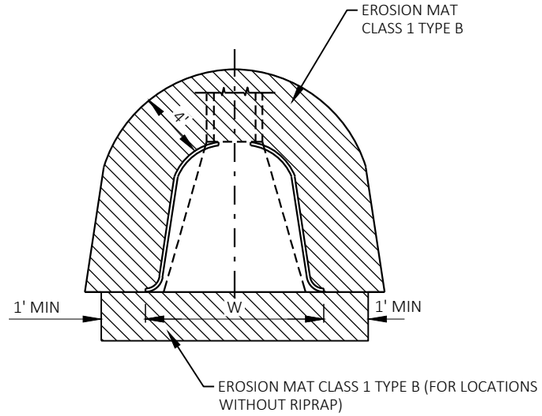


SECTION B-B



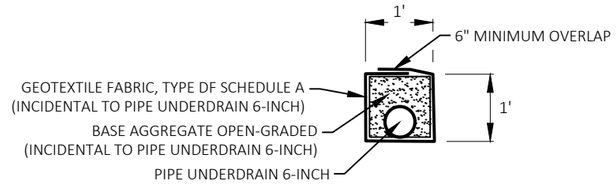
DETAIL FOR RELIEF TRENCHES

TRENCHES ARE TO BE CONSTRUCTED AT LEAST EVERY 250' AND AT EACH SAG VERTICAL CURVE IN THE PROFILE. LOCATIONS TO BE DETERMINED IN THE FIELD BY THE ENGINEER. EXCAVATION REQUIRED TO CONSTRUCT RELIEF TRENCHES SHALL BE CONSIDERED INCIDENTAL TO THE ITEM "SELECT CRUSHED MATERIAL".

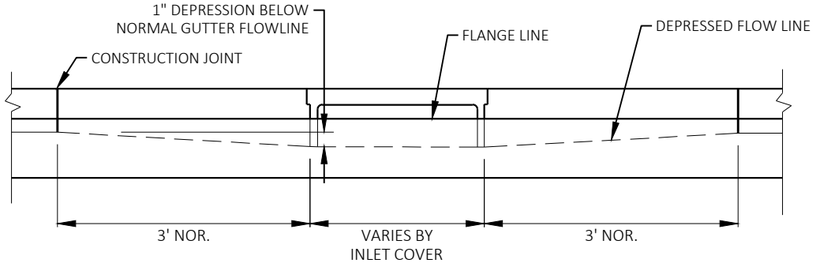


EROSION MAT AT APRON ENDWALLS

(SEE EROSION CONTROL PLANS FOR APPROXIMATE LOCATIONS)

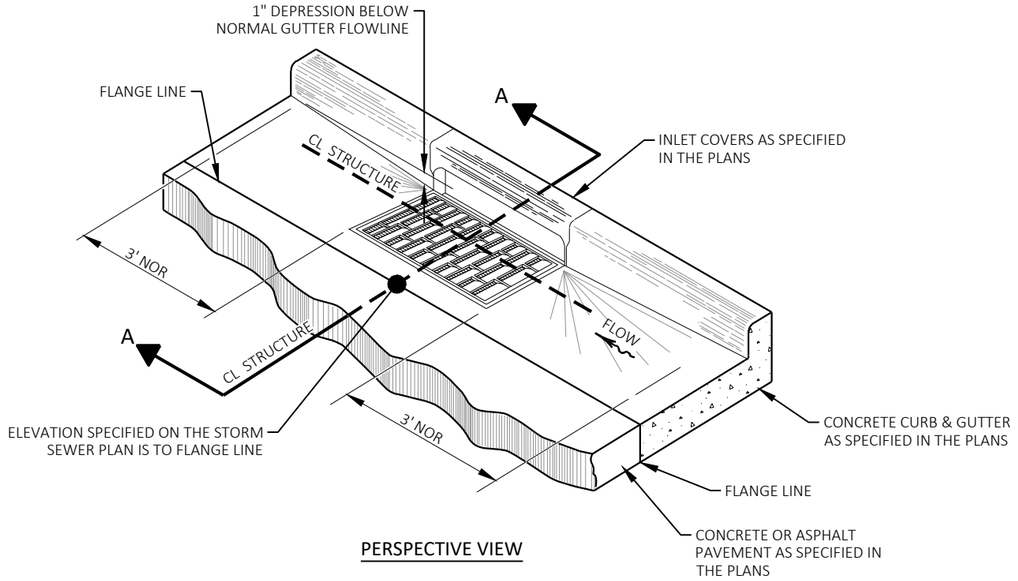


PIPE UNDERDRAIN TRENCH DETAIL

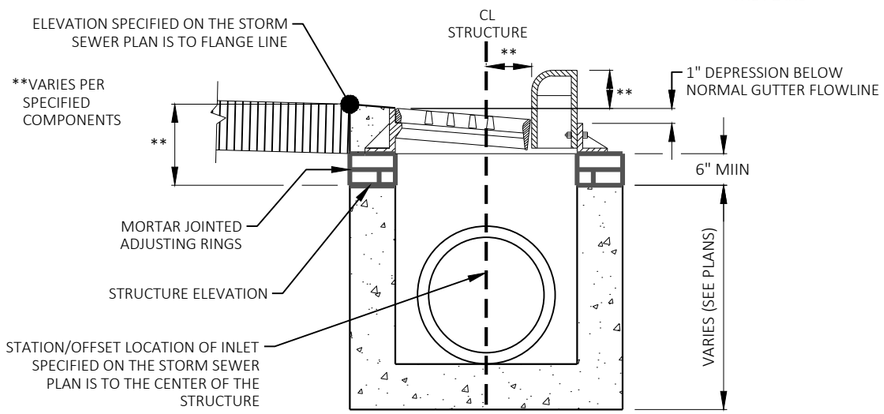


ELEVATION VIEW AT FLANGE LINE

(GRATE AND CURB DETAILS OMITTED FOR DRAWING CLARITY)



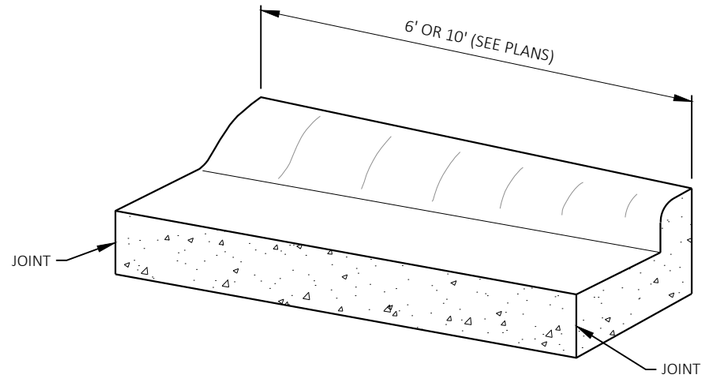
PERSPECTIVE VIEW



SECTION A-A

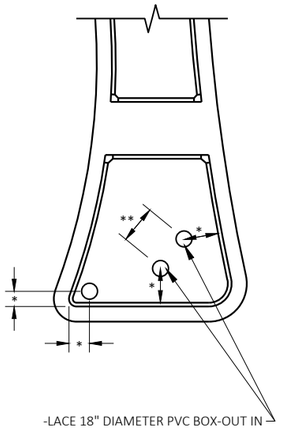
NOTE:
INLET COVER - TYPE H AND CONCRETE CURB AND GUTTER 30-INCH SHOWN FOR DETAIL ILLUSTRATION ONLY. GUTTER PAN DEPRESSION IS SIMILAR FOR OTHER CURB AND GUTTER /INLET COMBINATIONS

DETAIL OF CURB AND GUTTER AT INLETS



CURB TRANSITION DETAIL

CONCRETE CURB & GUTTER 4-INCH SLOPED 36-INCH TYPE D TO CONCRETE CURB & GUTTER 30-INCH TYPE D
(TO BE MEASURED AND PAID FOR AS CONCRETE CURB & GUTTER 36-INCH)

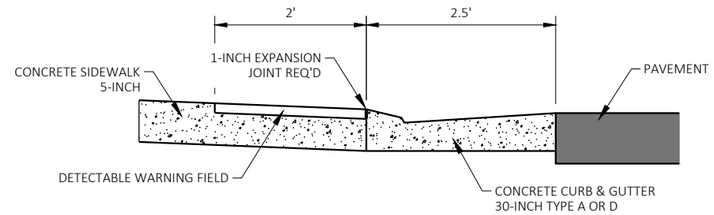


-LACE 18" DIAMETER PVC BOX-OUT IN CONCRETE FOR SIGN POSTS (TYP). THE NUMBER OF BOX-OUTS REQUIRED VARIES BY THE WIDTH OF THE SIGN, AND SHALL BE VERIFIED BEFORE PLACING CONCRETE IN THE ISLAND.

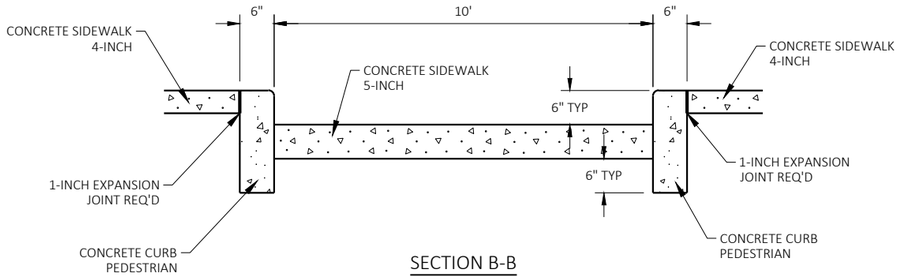
ISLAND SIGN LOCATION DETAIL (TYP)

* DISTANCE TO BE LAID OUT IN THE FIELD BASED ON SIGN SIZE. TWO FOOT MINIMUM CLEARANCE BETWEEN THE EDGE OF SIGN AND THE FACE OF CURB.

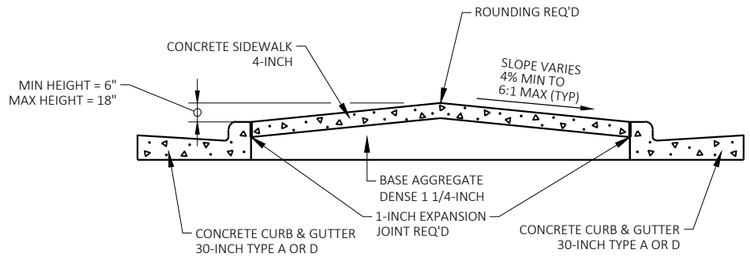
** SEE A4-4 SIGN PLATE FOR POST SPACING REQUIREMENTS.



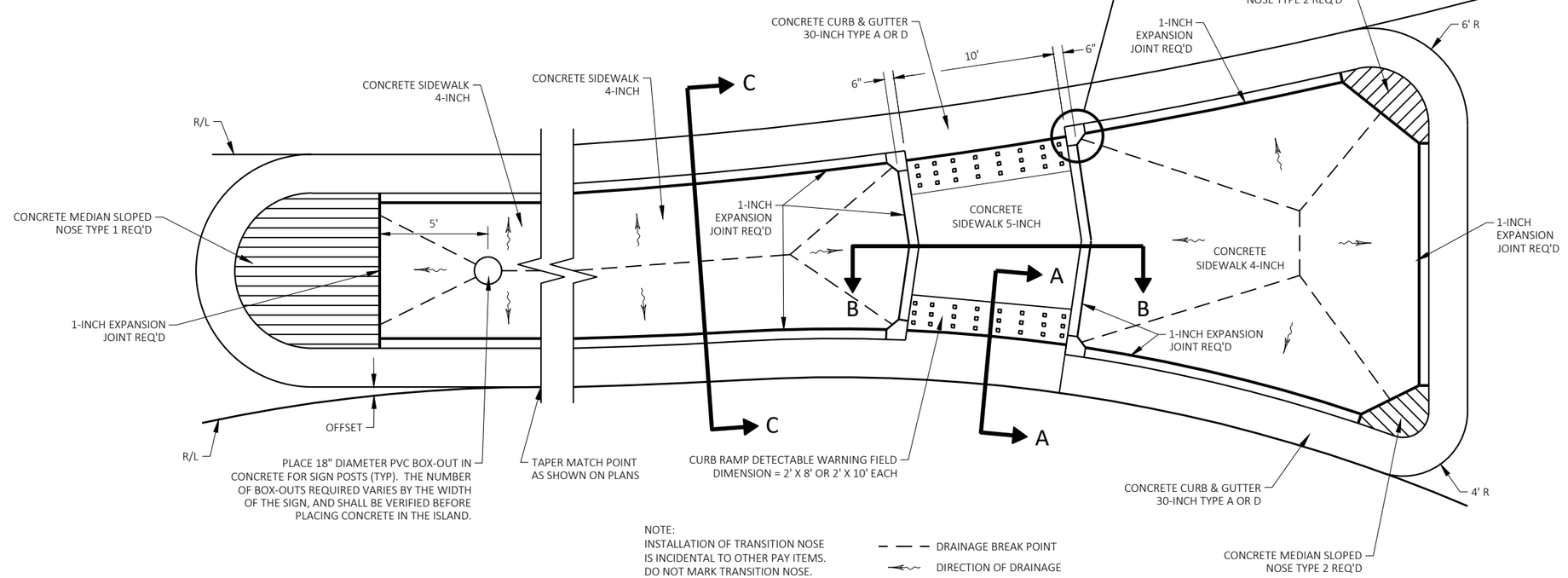
SECTION A-A



SECTION B-B



SECTION C-C



NOTE:
INSTALLATION OF TRANSITION NOSE
IS INCIDENTAL TO OTHER PAY ITEMS.
DO NOT MARK TRANSITION NOSE.

DRAINAGE BREAK POINT

DIRECTION OF DRAINAGE

SPLITTER ISLAND DETAIL

NOT TO SCALE

2

N

T

SEPC

SEPC

A/C

A/C

RESTAURANT

PARKING LOT

WELL

PARKING LOT

APPROXIMATELY 180' SAFETY FENCE
(IN PLACE DURING ROADWAY CONSTRUCTION)

PARKING LOT
ENTRANCE

CTH J

GUY

MP

STH 11

STH 11

PT: 209+74.50

PC: 210+22.98

210

SAFETY FENCE DETAIL

2

PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	CONSTRUCTION DETAILS	SHEET 12	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\021001-CD.DWG
LAYOUT NAME - 06

PLOT DATE : 12/18/2023 3:38 PM PLOT BY : KUSCHEL, LEVI PLOT NAME : PLOT SCALE : 1 IN:20 FT

WISDOT/CADDS SHEET 42

2

N

SLOPE INTERCEPT (TYP)

STH 11

207'EB'

STH 11

REMOVING SMALL PIPE CULVERTS

CTH J

155'NB'

A/C

WELL

REMOVING SMALL PIPE CULVERTS

SEPC

SEPC

154'NB'

REMOVING SMALL PIPE CULVERTS

REMOVING CURB & GUTTER (147')

WELL

REMOVING ASPHALTIC SURFACE (76 SQ. YD.)

REMOVING STORM SEWER (88' OF 18-INCH)

SAWCUT 122'

153'NB'

REMOVING GUARDRAIL (192')

REMOVING MANHOLE

REMOVING CURB & GUTTER (59')

REMOVING STORM SEWER (132' OF 18-INCH)

STH 11

208'EB'

209'EB'

210'EB'

REMOVING CURB & GUTTER (59')

REMOVING SMALL PIPE CULVERTS

GUY

MP

CTH J

151'NB'

REMOVING CURB & GUTTER (76')

MP

MP

LEGEND:

= REMOVING DELINEATORS AND MARKERS

NOTE:
SIGN REMOVALS ARE COVERED IN THE SIGNING PLAN.

N

CTH J

150'NB'

149'NB'

REMOVING SMALL PIPE CULVERTS

MP

SLOPE INTERCEPT (TYP)

GUY

GUY

GUY

SLOPE INTERCEPT (TYP)

MP

215'EB'

214'EB'

213'EB'

N

217'EB'

STH 11

218'EB'

219'E

REMOVING CULVERT PIPE - 70' OF EXISTING 164'.
(CUTTING OF EXISTING PIPE IS INCIDENTAL)

2

PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	REMOVAL PLAN	SHEET 13	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\021101-RP.DWG
LAYOUT NAME - 01

PLOT DATE : 10/4/2023 2:48 PM

PLOT BY : KUSCHEL, LEVI

PLOT NAME :

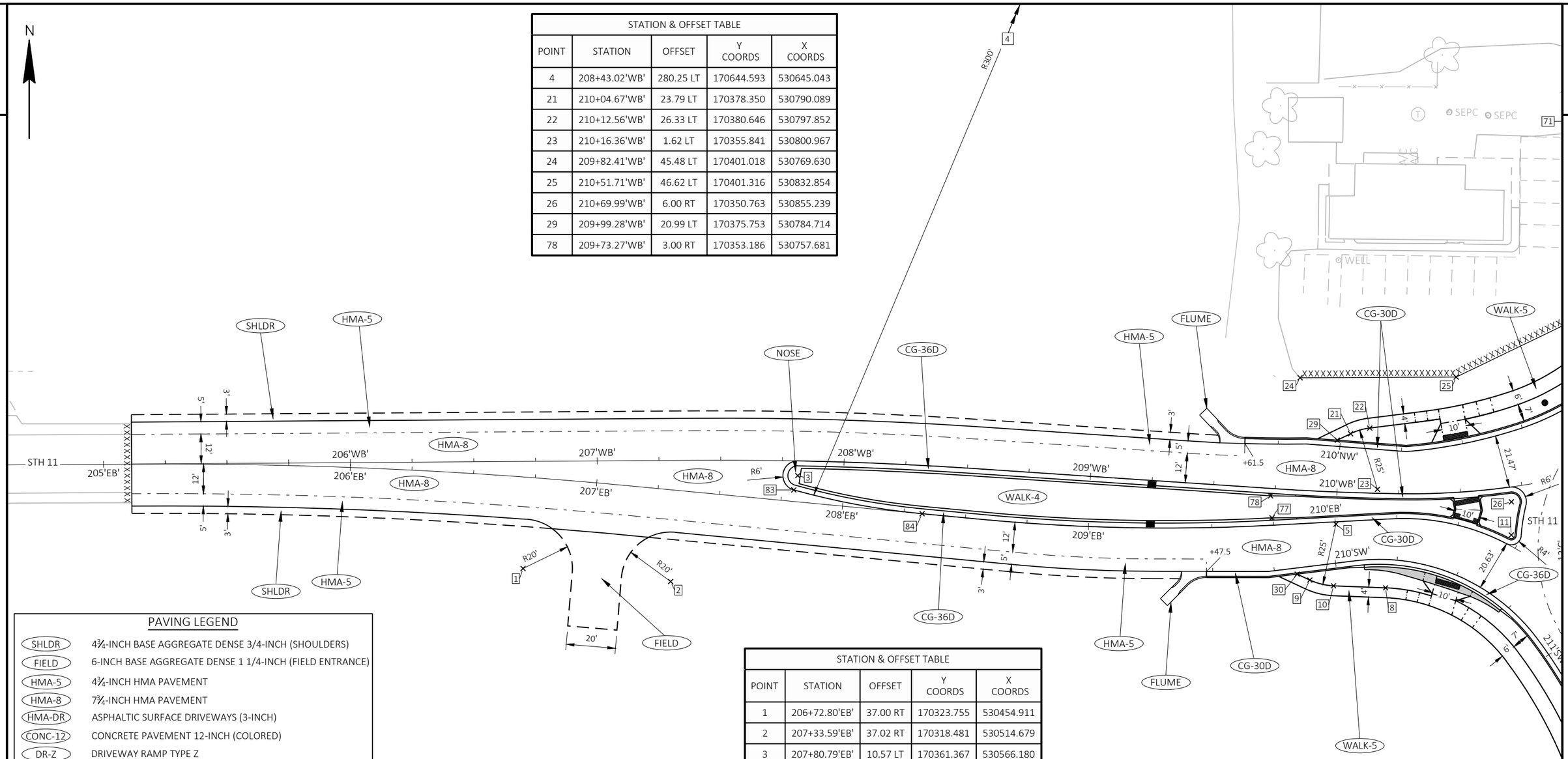
PLOT SCALE : 1 IN:50 FT

WISDOT/CADDS SHEET 42

2

N

STATION & OFFSET TABLE				
POINT	STATION	OFFSET	Y COORDS	X COORDS
4	208+43.02'WB'	280.25 LT	170644.593	530645.043
21	210+04.67'WB'	23.79 LT	170378.350	530790.089
22	210+12.56'WB'	26.33 LT	170380.646	530797.852
23	210+16.36'WB'	1.62 LT	170355.841	530800.967
24	209+82.41'WB'	45.48 LT	170401.018	530769.630
25	210+51.71'WB'	46.62 LT	170401.316	530832.854
26	210+69.99'WB'	6.00 RT	170350.763	530855.239
29	209+99.28'WB'	20.99 LT	170375.753	530784.714
78	209+73.27'WB'	3.00 RT	170353.186	530757.681



PAVING LEGEND

SHLDR	4¾-INCH BASE AGGREGATE DENSE 3/4-INCH (SHOULDERS)
FIELD	6-INCH BASE AGGREGATE DENSE 1 1/4-INCH (FIELD ENTRANCE)
HMA-5	4¾-INCH HMA PAVEMENT
HMA-8	7¾-INCH HMA PAVEMENT
HMA-DR	ASPHALTIC SURFACE DRIVEWAYS (3-INCH)
CONC-12	CONCRETE PAVEMENT 12-INCH (COLORED)
DR-Z	DRIVEWAY RAMP TYPE Z
DRV-6	CONCRETE DRIVEWAY 6-INCH
CG-18A	CONCRETE CURB & GUTTER 18-INCH TYPE A (REVERSE)
CG-30D	CONCRETE CURB & GUTTER 30-INCH TYPE D
CG-36D	CONCRETE CURB & GUTTER 4-INCH SLOPED 36-INCH TYPE D
CG-36T	CONCRETE CURB & GUTTER 4-INCH SLOPED 36-INCH TYPE T
NOSE	CONCRETE MEDIAN SLOPE NOSE
WALK-4	CONCRETE SIDEWALK 4-INCH
WALK-5	CONCRETE SIDEWALK 5-INCH (SIDEPATH)
P-CURB	CONCRETE CURB PEDESTRIAN
FLUME	ASPHALTIC FLUME
▬▬	CURB RAMP DETECTABLE WARNING FIELD

STATION & OFFSET TABLE				
POINT	STATION	OFFSET	Y COORDS	X COORDS
1	206+72.80'EB'	37.00 RT	170323.755	530454.911
2	207+33.59'EB'	37.02 RT	170318.481	530514.679
3	207+80.79'EB'	10.57 LT	170361.367	530566.180
5	210+00.25'EB'	0.95 RT	170341.720	530784.177
8	210+18.92'EB'	27.79 RT	170315.918	530804.259
9	209+88.44'EB'	22.99 RT	170319.081	530773.572
10	209+97.94'EB'	25.84 RT	170316.739	530783.203
11	210+71.15'EB'	4.00 LT	170337.327	530855.188
30	209+83.47'EB'	20.33 RT	170321.474	530768.463
77	209+74.50'EB'	3.00 LT	170344.286	530758.251
83	207+79.74'EB'	4.66 LT	170355.587	530564.571
84	208+32.40'EB'	0.00 RT	170345.949	530616.553

NOTES
REVIEW SIGNING PLAN FOR SIGN BOX-OUT LOCATIONS

PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	PLAN DETAILS	SHEET 14	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\021201-PD.DWG
LAYOUT NAME - 01
PLOT DATE : 10/27/2023 11:29 AM PLOT BY : KUSCHEL, LEVI PLOT NAME : PLOT SCALE : 1 IN:40 FT
WISDOT/CADDS SHEET 42

2

NOTES

REVIEW SIGNING PLAN FOR SIGN BOX-OUT LOCATIONS

N

STATION & OFFSET TABLE				
POINT	STATION	OFFSET	Y COORDS	X COORDS
44	212+39.66'NE'	4.00 LT	170366.118	531018.844
45	212+55.27'NE'	5.76 LT	170362.207	531032.992
46	212+84.91'NE'	5.74 LT	170359.805	531060.076
47	213+00.45'NE'	4.00 LT	170361.188	531074.778
48	212+26.28'NE'	53.51 LT	170411.833	531039.098
49	213+02.34'NE'	53.96 LT	170409.731	531062.797

STATION & OFFSET TABLE				
POINT	STATION	OFFSET	Y COORDS	X COORDS
6	210+06.34'SW'	53.83 RT	170273.258	530796.911
7	210+81.13'SW'	53.79 RT	170265.850	530820.274
14	210+27.07'SW'	5.53 RT	170321.419	530810.345
15	210+11.46'SW'	4.00 RT	170323.244	530795.726
16	210+57.34'SW'	5.53 RT	170312.953	530837.046
18	210+72.95'SW'	4.00 RT	170306.021	530850.044

PAVING LEGEND

SHLDR	4¾-INCH BASE AGGREGATE DENSE 3/4-INCH (SHOULDERS)	CG-30D	CONCRETE CURB & GUTTER 30-INCH TYPE D
FIELD	6-INCH BASE AGGREGATE DENSE 1 1/4-INCH (FIELD ENTRANCE)	CG-36D	CONCRETE CURB & GUTTER 4-INCH SLOPED 36-INCH TYPE D
HMA-5	4¾-INCH HMA PAVEMENT	CG-36T	CONCRETE CURB & GUTTER 4-INCH SLOPED 36-INCH TYPE T
HMA-8	7¾-INCH HMA PAVEMENT	NOSE	CONCRETE MEDIAN SLOPE NOSE
HMA-DR	HMA DRIVEWAY (3-INCH)	WALK-4	CONCRETE SIDEWALK 4-INCH
CONC-12	CONCRETE PAVEMENT 12-INCH (COLORED)	WALK-5	CONCRETE SIDEWALK 5-INCH (SIDEPATH)
DR-Z	DRIVEWAY RAMP TYPE Z	P-CURB	CONCRETE CURB PEDESTRIAN
DRV-6	CONCRETE DRIVEWAY 6-INCH	FLUME	ASPHALTIC FLUME
CG-18A	CONCRETE CURB & GUTTER 18-INCH TYPE A (REVERSE)		CURB RAMP DETECTABLE WARNING FIELD

PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	PLAN DETAILS	SHEET 15	E

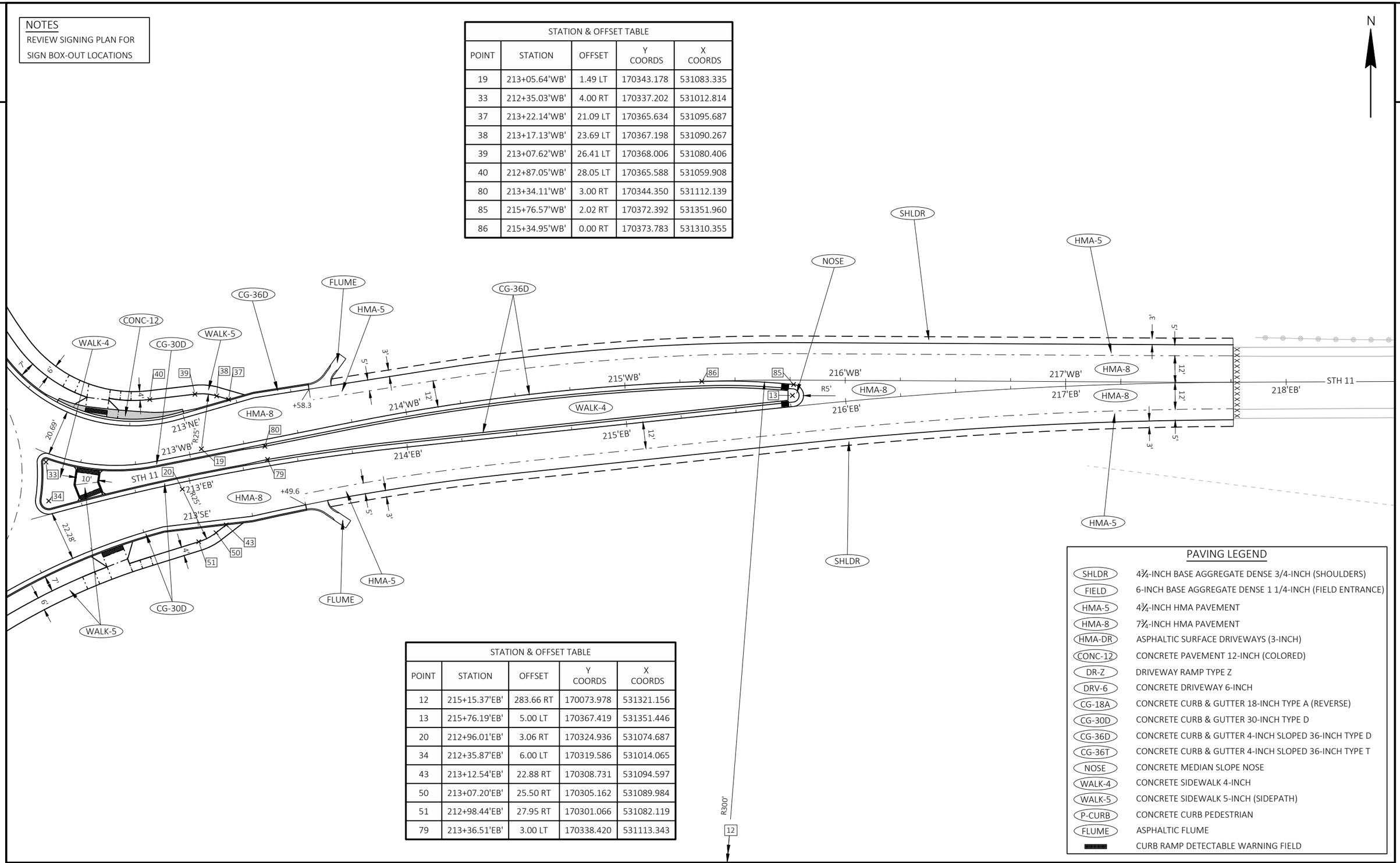
FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\021201-PD.DWG
LAYOUT NAME - 02

PLOT DATE : 10/27/2023 11:30 AM PLOT BY : KUSCHEL, LEVI PLOT NAME : PLOT SCALE : 1 IN:20 FT

WISDOT/CADDS SHEET 42

NOTES
REVIEW SIGNING PLAN FOR
SIGN BOX-OUT LOCATIONS

STATION & OFFSET TABLE				
POINT	STATION	OFFSET	Y COORDS	X COORDS
19	213+05.64'WB'	1.49 LT	170343.178	531083.335
33	212+35.03'WB'	4.00 RT	170337.202	531012.814
37	213+22.14'WB'	21.09 LT	170365.634	531095.687
38	213+17.13'WB'	23.69 LT	170367.198	531090.267
39	213+07.62'WB'	26.41 LT	170368.006	531080.406
40	212+87.05'WB'	28.05 LT	170365.588	531059.908
80	213+34.11'WB'	3.00 RT	170344.350	531112.139
85	215+76.57'WB'	2.02 RT	170372.392	531351.960
86	215+34.95'WB'	0.00 RT	170373.783	531310.355



STATION & OFFSET TABLE				
POINT	STATION	OFFSET	Y COORDS	X COORDS
12	215+15.37'EB'	283.66 RT	170073.978	531321.156
13	215+76.19'EB'	5.00 LT	170367.419	531351.446
20	212+96.01'EB'	3.06 RT	170324.936	531074.687
34	212+35.87'EB'	6.00 LT	170319.586	531014.065
43	213+12.54'EB'	22.88 RT	170308.731	531094.597
50	213+07.20'EB'	25.50 RT	170305.162	531089.984
51	212+98.44'EB'	27.95 RT	170301.066	531082.119
79	213+36.51'EB'	3.00 LT	170338.420	531113.343

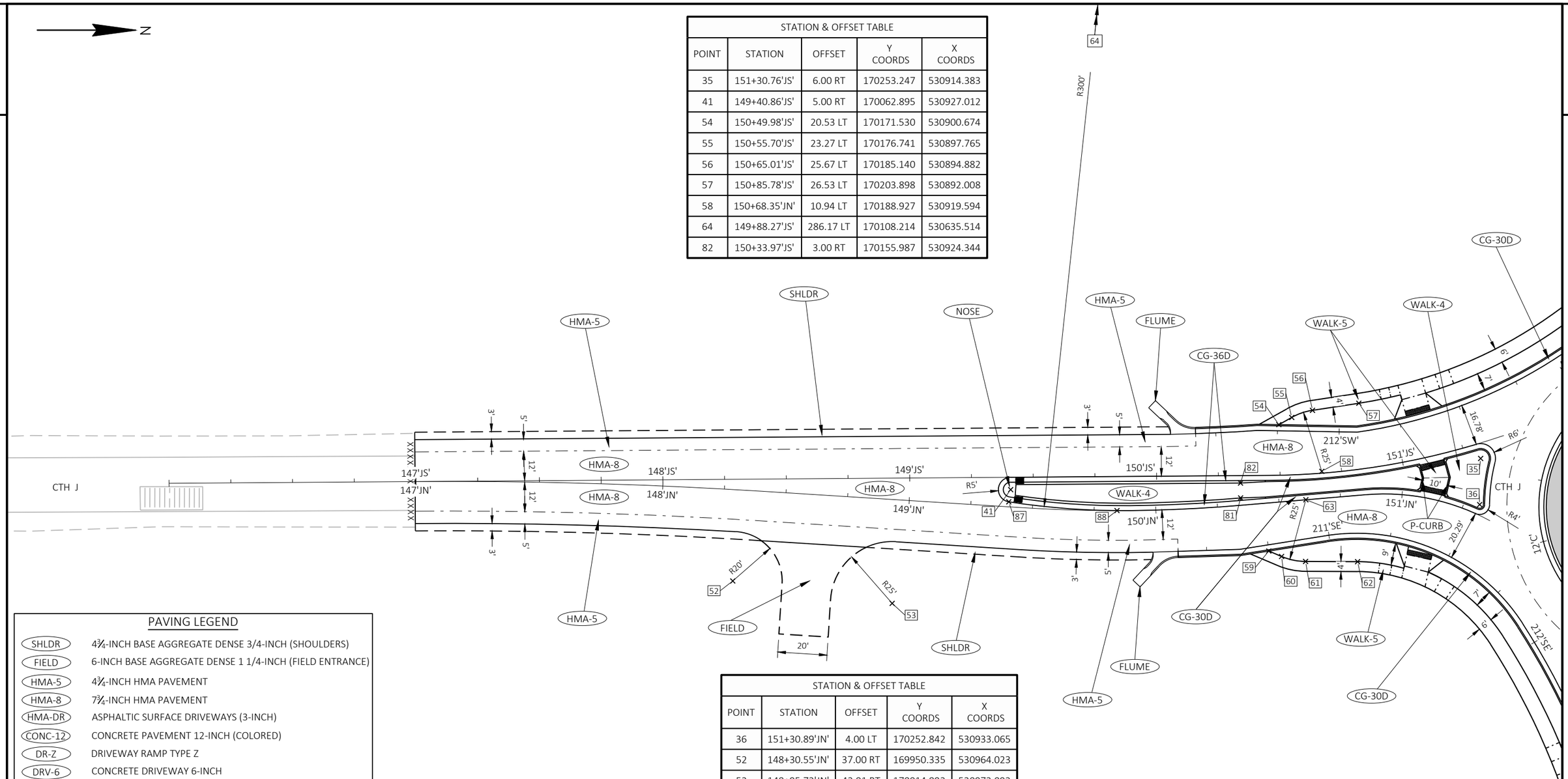
PAVING LEGEND

SHLDR	4¾-INCH BASE AGGREGATE DENSE 3/4-INCH (SHOULDERS)
FIELD	6-INCH BASE AGGREGATE DENSE 1 1/4-INCH (FIELD ENTRANCE)
HMA-5	4¾-INCH HMA PAVEMENT
HMA-8	7¾-INCH HMA PAVEMENT
HMA-DR	ASPHALTIC SURFACE DRIVEWAYS (3-INCH)
CONC-12	CONCRETE PAVEMENT 12-INCH (COLORED)
DR-Z	DRIVEWAY RAMP TYPE Z
DRV-6	CONCRETE DRIVEWAY 6-INCH
CG-18A	CONCRETE CURB & GUTTER 18-INCH TYPE A (REVERSE)
CG-30D	CONCRETE CURB & GUTTER 30-INCH TYPE D
CG-36D	CONCRETE CURB & GUTTER 4-INCH SLOPED 36-INCH TYPE D
CG-36T	CONCRETE CURB & GUTTER 4-INCH SLOPED 36-INCH TYPE T
NOSE	CONCRETE MEDIAN SLOPE NOSE
WALK-4	CONCRETE SIDEWALK 4-INCH
WALK-5	CONCRETE SIDEWALK 5-INCH (SIDEPATH)
P-CURB	CONCRETE CURB PEDESTRIAN
FLUME	ASPHALTIC FLUME
	CURB RAMP DETECTABLE WARNING FIELD

2

N

STATION & OFFSET TABLE				
POINT	STATION	OFFSET	Y COORDS	X COORDS
35	151+30.76'JS'	6.00 RT	170253.247	530914.383
41	149+40.86'JS'	5.00 RT	170062.895	530927.012
54	150+49.98'JS'	20.53 LT	170171.530	530900.674
55	150+55.70'JS'	23.27 LT	170176.741	530897.765
56	150+65.01'JS'	25.67 LT	170185.140	530894.882
57	150+85.78'JS'	26.53 LT	170203.898	530892.008
58	150+68.35'JN'	10.94 LT	170188.927	530919.594
64	149+88.27'JS'	286.17 LT	170108.214	530635.514
82	150+33.97'JS'	3.00 RT	170155.987	530924.344



PAVING LEGEND

SHLDR	4¾-INCH BASE AGGREGATE DENSE 3/4-INCH (SHOULDERS)
FIELD	6-INCH BASE AGGREGATE DENSE 1 1/4-INCH (FIELD ENTRANCE)
HMA-5	4¾-INCH HMA PAVEMENT
HMA-8	7¾-INCH HMA PAVEMENT
HMA-DR	ASPHALTIC SURFACE DRIVEWAYS (3-INCH)
CONC-12	CONCRETE PAVEMENT 12-INCH (COLORED)
DR-Z	DRIVEWAY RAMP TYPE Z
DRV-6	CONCRETE DRIVEWAY 6-INCH
CG-18A	CONCRETE CURB & GUTTER 18-INCH TYPE A (REVERSE)
CG-30D	CONCRETE CURB & GUTTER 30-INCH TYPE D
CG-36D	CONCRETE CURB & GUTTER 4-INCH SLOPED 36-INCH TYPE D
CG-36T	CONCRETE CURB & GUTTER 4-INCH SLOPED 36-INCH TYPE T
NOSE	CONCRETE MEDIAN SLOPE NOSE
WALK-4	CONCRETE SIDEWALK 4-INCH
WALK-5	CONCRETE SIDEWALK 5-INCH (SIDEPATH)
P-CURB	CONCRETE CURB PEDESTRIAN
FLUME	ASPHALTIC FLUME
▬	CURB RAMP DETECTABLE WARNING FIELD

STATION & OFFSET TABLE				
POINT	STATION	OFFSET	Y COORDS	X COORDS
36	151+30.89'JN'	4.00 LT	170252.842	530933.065
52	148+30.55'JN'	37.00 RT	169950.335	530964.023
53	148+95.72'JN'	42.01 RT	170014.892	530973.092
59	150+44.20'JN'	19.41 RT	170167.442	530951.887
60	150+49.18'JN'	22.06 RT	170172.630	530954.104
61	150+58.68'JN'	24.89 RT	170182.335	530956.114
62	150+79.67'JN'	26.78 RT	170203.408	530956.213
63	150+60.92'JN'	0.01 LT	170182.453	530931.114
81	150+34.65'JN'	3.00 LT	170156.030	530930.369
87	149+40.27'JN'	2.02 LT	170062.127	530931.953
88	149+84.28'JN'	0.00 RT	170105.967	530935.505

NOTES
REVIEW SIGNING PLAN FOR SIGN BOX-OUT LOCATIONS

PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	PLAN DETAILS	SHEET 17	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\021201-PD.DWG
LAYOUT NAME - 04

PLOT DATE : 10/27/2023 11:30 AM PLOT BY : KUSCHEL, LEVI PLOT NAME : PLOT SCALE : 1 IN:40 FT

WISDOT/CADDS SHEET 42

2

NOTES

REVIEW SIGNING PLAN FOR SIGN BOX-OUT LOCATIONS

N

STATION & OFFSET TABLE				
POINT	STATION	OFFSET	Y COORDS	X COORDS
27	153+55.01'JS'	25.59 LT	170467.717	530902.513
28	153+89.98'JS'	24.21 LT	170502.565	530899.250
31	152+95.26'JS'	4.00 RT	170410.768	530930.723
42	153+31.89'JS'	27.38 LT	170444.942	530903.756
65	153+49.91'JS'	4.00 RT	170466.589	530932.520
66	154+52.47'JS'	4.00 RT	170568.166	530920.398
67	154+70.27'JS'	27.14 LT	170584.280	530888.206
70	154+70.49'JS'	42.61 LT	170583.733	530872.746
71	153+94.67'JS'	42.41 LT	170504.800	530880.581
75	152+95.35'JS'	43.85 LT	170426.722	530885.608
93	154+52.47'JS'	0.00 RT	170567.873	530916.409
94	154+85.62'JS'	2.04 RT	170601.038	530916.729

~2" DIA. PVC DRAIN PIPE ORIGINATING FROM RESTAURANT TO REMAIN. DO NOT DISTURB.

STATION & OFFSET TABLE				
POINT	STATION	OFFSET	Y COORDS	X COORDS
32	152+95.57'JN'	6.00 LT	170411.734	530947.312
58	150+68.35'JN'	10.94 LT	170188.927	530919.594
69	154+85.26'JN'	5.00 LT	170600.575	530921.708
72	153+68.06'JN'	19.98 RT	170486.562	530958.611
73	153+62.79'JN'	22.80 RT	170481.617	530961.956
74	153+54.67'JN'	25.37 RT	170474.265	530965.345
76	153+50.97'JN'	0.62 RT	170467.611	530941.247
89	153+35.54'JN'	0.00 RT	170452.293	530943.075
90	153+53.04'JN'	4.68 LT	170468.994	530935.716
91	154+51.01'JN'	6.26 LT	170566.376	530923.975
92	154+72.50'JN'	0.00 RT	170588.394	530927.994

PAVING LEGEND

SHLDR	4¾-INCH BASE AGGREGATE DENSE 3/4-INCH (SHOULDERS)
FIELD	6-INCH BASE AGGREGATE DENSE 1 1/4-INCH (FIELD ENTRANCE)
HMA-5	4¾-INCH HMA PAVEMENT
HMA-8	7¾-INCH HMA PAVEMENT
HMA-DR	ASPHALTIC SURFACE DRIVEWAYS (3-INCH)
CONC-12	CONCRETE PAVEMENT 12-INCH (COLORED)
DR-Z	DRIVEWAY RAMP TYPE Z
DRV-6	CONCRETE DRIVEWAY 6-INCH
CG-18A	CONCRETE CURB & GUTTER 18-INCH TYPE A (REVERSE)
CG-30D	CONCRETE CURB & GUTTER 30-INCH TYPE D
CG-36D	CONCRETE CURB & GUTTER 4-INCH SLOPED 36-INCH TYPE D
CG-36T	CONCRETE CURB & GUTTER 4-INCH SLOPED 36-INCH TYPE T
NOSE	CONCRETE MEDIAN SLOPE NOSE
WALK-4	CONCRETE SIDEWALK 4-INCH
WALK-5	CONCRETE SIDEWALK 5-INCH (SIDEPATH)
P-CURB	CONCRETE CURB PEDESTRIAN
FLUME	ASPHALTIC FLUME
	CURB RAMP DETECTABLE WARNING FIELD

PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	PLAN DETAILS	SHEET 18	E

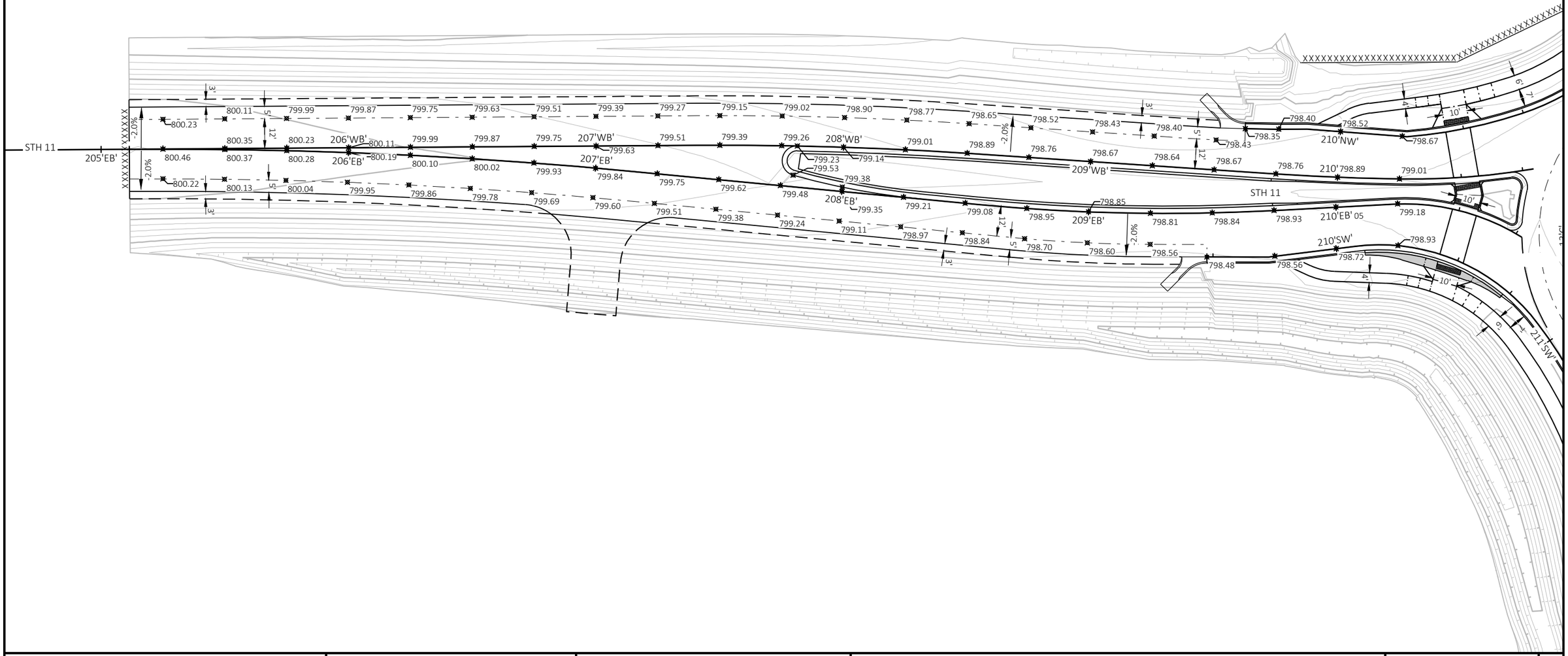
FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\021201-PD.DWG
LAYOUT NAME - 05

PLOT DATE : 10/27/2023 11:30 AM PLOT BY : KUSCHEL, LEVI PLOT NAME : PLOT SCALE : 1 IN:40 FT

WISDOT/CADDS SHEET 42

2

N



PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	PAVING GRADES	SHEET 19	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\021202-PD (GRADES).DWG
LAYOUT NAME - 01

PLOT DATE : 10/27/2023 11:39 AM PLOT BY : KUSCHEL, LEVI PLOT NAME : PLOT SCALE : 1 IN:40 FT

2

N

153'SB'
153'NB'
CTH J
211'NW'
212'NE'
213'NE'
210'NW'
210'WB'
213'WB'
STH 11
210'EB'
213'EB'
210'SW'
211'SW'
212'SE'
213'SE'
11'C'
12'C'
13'C'
22'CC'
LOW POINT
HIGH POINT
798.80
798.82
798.80
798.59
798.61
798.71
798.44
798.82
799.01
798.95
798.70
798.52
798.81
798.63
799.20
798.51
798.95
798.83
799.00
798.48
798.71
799.35
799.18
799.40
798.91
798.52
799.14
799.24
799.16
799.49
798.32
799.33
799.30
799.40
798.94
798.66
799.40
799.54
799.34
799.74
798.65
798.84
798.73
798.59
799.19
798.83
798.82
798.75
799.49
799.90
799.87
799.51
799.03
798.82
799.35
799.68
798.55
799.50
799.48
799.02
798.80
799.70
799.40
799.21
799.22
799.47
799.59
799.40
799.50
799.41
799.11
799.49
799.34
799.46
799.31
799.07
799.31
6'
4'
-1.5%
-2.0%
-1.0%
-1.7%
-2.5%
20'
13'
11'
10'
7'

PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	PAVING GRADES	SHEET 20 E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\021202-PD (GRADES).DWG
LAYOUT NAME - 02

PLOT DATE : 10/27/2023 11:39 AM PLOT BY : KUSCHEL, LEVI PLOT NAME : PLOT SCALE : 1 IN:20 FT

2

2

N

213'NE'
213'WB'
213'EB'
213'SE'
214'WB'
214'EB'
215'WB'
215'EB'
216'WB'
216'EB'
217'WB'
217'EB'
218'EB'
STH 11

PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	PAVING GRADES	SHEET 21 E

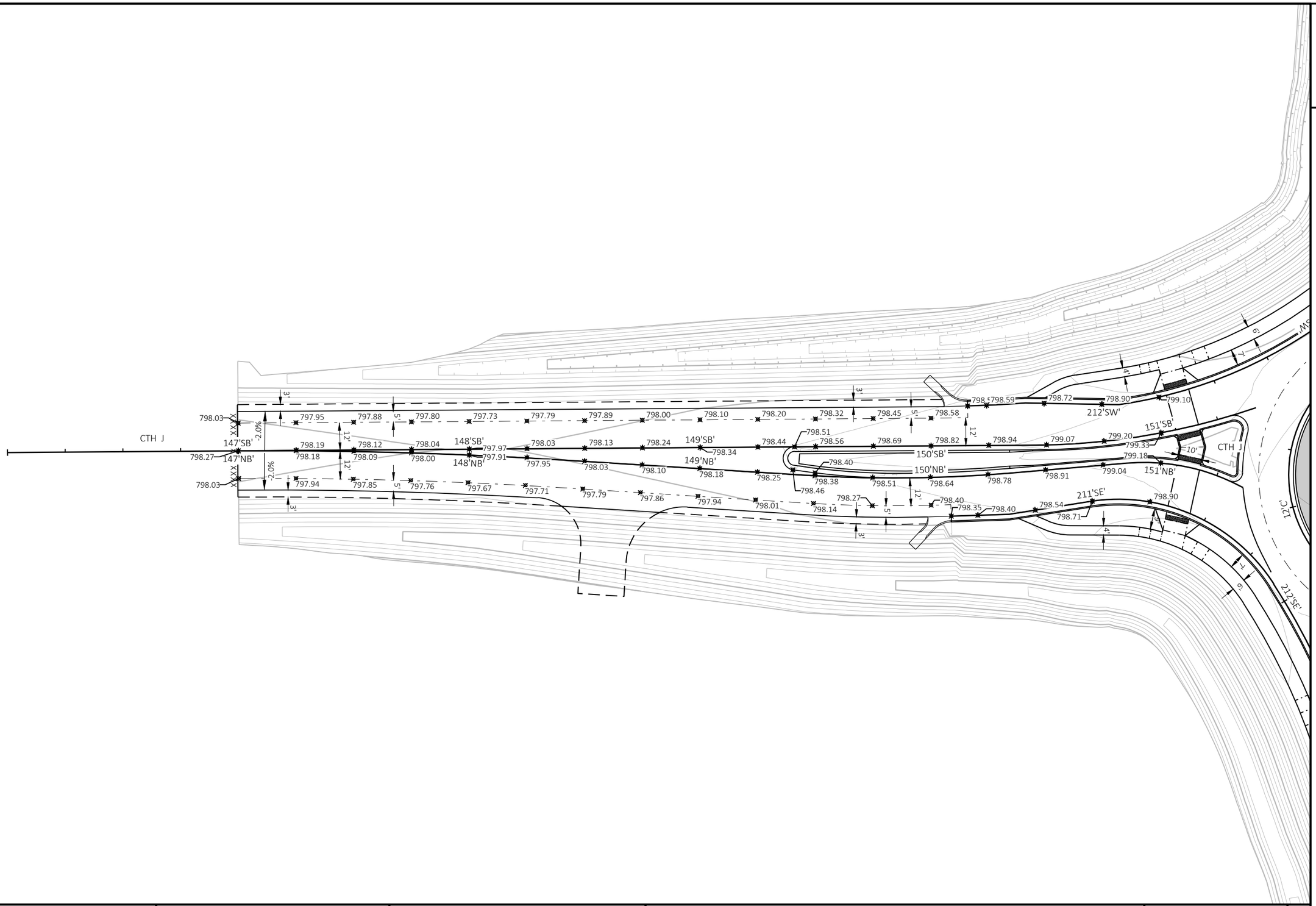
FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\021202-PD (GRADES).DWG
LAYOUT NAME - 03

PLOT DATE : 10/27/2023 11:39 AM PLOT BY : KUSCHEL, LEVI PLOT NAME : PLOT SCALE : 1 IN:40 FT

WISDOT/CADDS SHEET 42

2

N



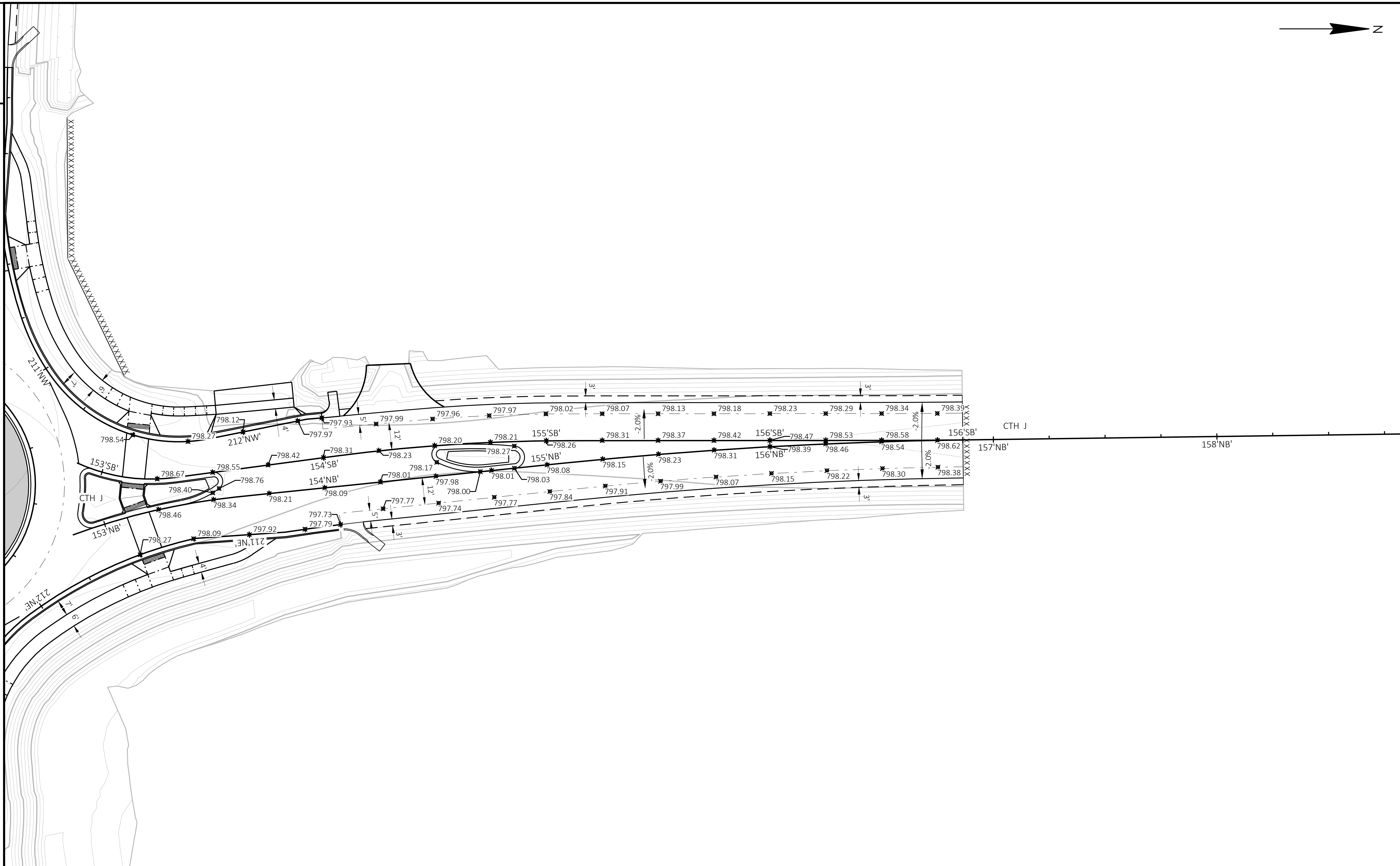
PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	PAVING GRADES	SHEET 22	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\021202-PD (GRADES).DWG
LAYOUT NAME - 04

PLOT DATE : 10/27/2023 11:39 AM PLOT BY : KUSCHEL, LEVI PLOT NAME : PLOT SCALE : 1 IN:40 FT

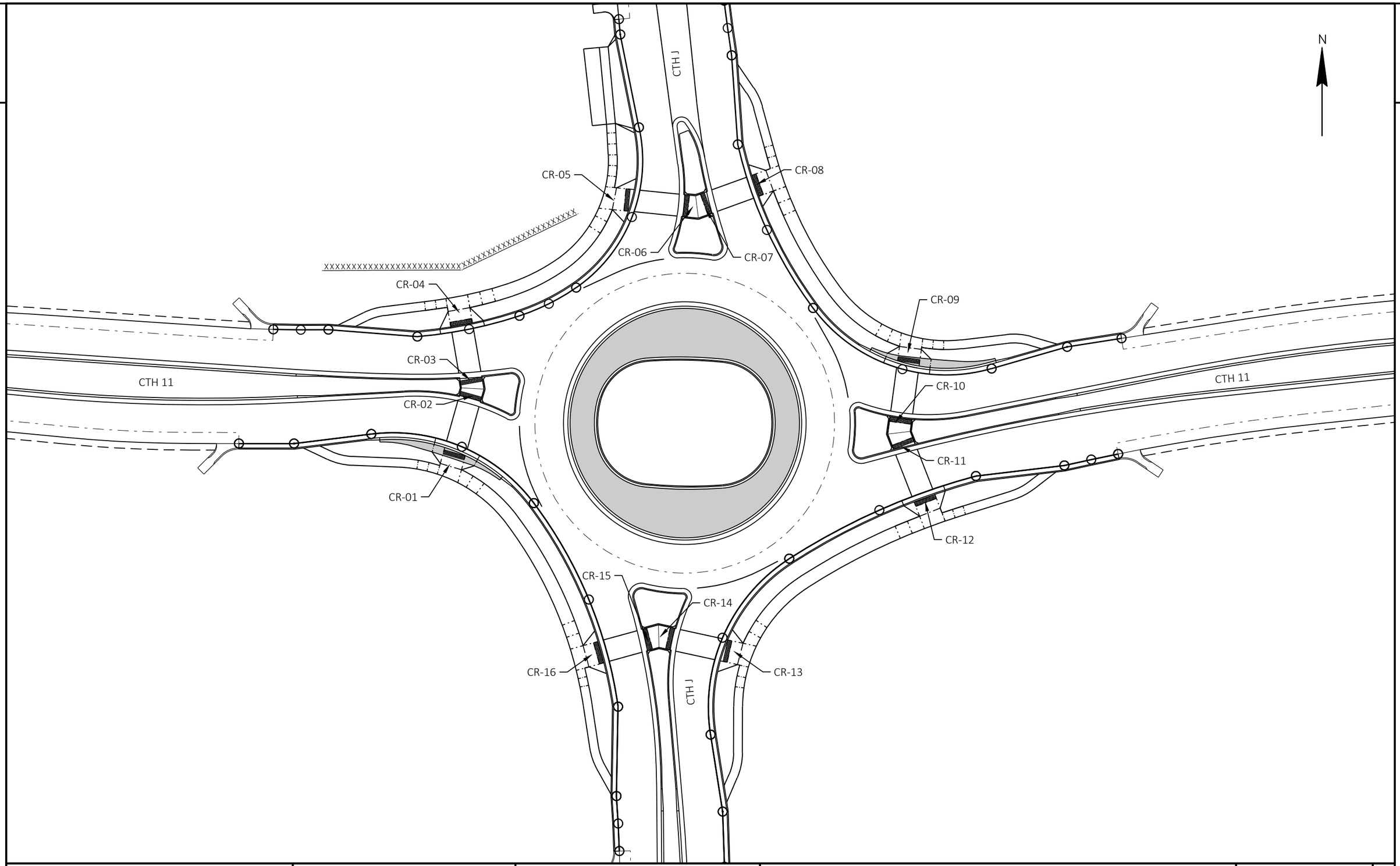
2

2



PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	PAVING GRADES	SHEET 23	E

2



PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	CURB RAMP DETAILS	SHEET 24	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\021301_CR.DWG
LAYOUT NAME - ov-1

PLOT DATE : 10/27/2023 11:46 AM PLOT BY : KUSCHEL, LEVI PLOT NAME : PLOT SCALE : 1 IN:40 FT

STATION & OFFSET TABLE				
POINT	STATION	OFFSET	Y COORDS	X COORDS
400	210+31.96'NW'	11.47 LT	170382.977	530816.416
401	210+37.27'NW'	11.50 LT	170383.606	530821.377
402	210+42.57'NW'	11.50 LT	170384.332	530826.324
403	210+42.60'NW'	9.63 LT	170382.487	530826.646
404	210+42.70'NW'	2.00 LT	170374.972	530827.950
405	210+42.70'NW'	0.00	170372.997	530828.268
406	210+52.83'NW'	0.00	170374.860	530838.223
407	210+52.83'NW'	2.00 LT	170376.816	530837.805
408	210+53.10'NW'	9.50 LT	170384.206	530836.497
409	210+59.40'NW'	9.50 LT	170385.559	530842.342

STATION & OFFSET TABLE				
POINT	STATION	OFFSET	Y COORDS	X COORDS
410	210+65.70'NW'	9.50 LT	170387.096	530848.142
411	210+31.88'NW'	15.47 LT	170386.946	530815.918
412	210+37.27'NW'	15.50 LT	170387.570	530820.848
413	210+42.52'NW'	15.50 LT	170388.273	530825.636
414	210+53.36'NW'	15.50 LT	170390.121	530835.465
415	210+59.40'NW'	15.50 LT	170391.383	530840.897
416	210+65.70'NW'	15.50 LT	170392.871	530846.514
417	210+65.70'NW'	15.50 LT	170377.557	530827.505
418	210+65.70'NW'	15.50 LT	170379.275	530837.356

STATION & OFFSET TABLE				
POINT	STATION	OFFSET	Y COORDS	X COORDS
200	210+47.45'EB'	0.00	170341.883	530831.139
201	210+57.24'EB'	0.00	170339.248	530840.560
202	210+47.45'EB'	2.00 LT	170343.835	530831.572
203	210+57.24'EB'	2.00 LT	170341.142	530841.202
204	210+47.66'EB'	6.15 LT	170347.838	530832.691
205	210+56.92'EB'	8.41 LT	170347.325	530842.932
206	210+46.03'EB'	10.56 LT	170352.527	530831.873
207	210+54.42'EB'	13.76 LT	170353.277	530841.893
208	210+45.58'EB'	12.50 LT	170354.524	530831.757
209	210+53.78'EB'	15.62 LT	170355.269	530841.711
210	210+53.78'EB'	15.62 LT	170343.679	530841.912
211	210+47.66'EB'	6.15 LT	170346.376	530832.282
212	210+53.78'EB'	15.62 LT	170350.808	530842.325

STATION & OFFSET TABLE				
POINT	STATION	OFFSET	Y COORDS	X COORDS
100	210+31.18'SW'	15.50 RT	170311.048	530812.457
101	210+37.02'SW'	15.50 RT	170310.086	530817.067
102	210+43.08'SW'	15.50 RT	170308.737	530821.757
103	210+55.59'SW'	15.50 RT	170304.848	530831.053
104	210+61.01'SW'	15.50 RT	170302.722	530834.869
105	210+67.67'SW'	15.50 RT	170299.761	530839.352
106	210+31.19'SW'	11.50 RT	170314.990	530813.138
107	210+37.03'SW'	11.50 RT	170313.968	530818.032
108	210+42.87'SW'	11.50 RT	170312.593	530822.835
109	210+42.82'SW'	10.50 RT	170313.552	530823.103

STATION & OFFSET TABLE				
POINT	STATION	OFFSET	Y COORDS	X COORDS
110	210+42.64'SW'	6.62 RT	170317.299	530824.151
111	210+42.45'SW'	2.00 RT	170321.748	530825.395
112	210+42.45'SW'	0.00	170323.653	530826.006
113	210+52.77'SW'	0.00	170319.882	530835.599
114	210+52.77'SW'	1.99 RT	170318.079	530834.753
115	210+53.50'SW'	6.02 RT	170314.144	530833.652
116	210+54.20'SW'	9.50 RT	170310.735	530832.699
117	210+61.01'SW'	9.50 RT	170307.862	530837.965
118	210+67.82'SW'	9.50 RT	170304.551	530842.968

CR-04
TYPE 2 RAMP
(10:1 CONCRETE FLARES)

CR-03
TYPE 5 RAMP

CR-02
TYPE 5 RAMP

CR-01
TYPE 2 RAMP
(10:1 CONCRETE FLARES)

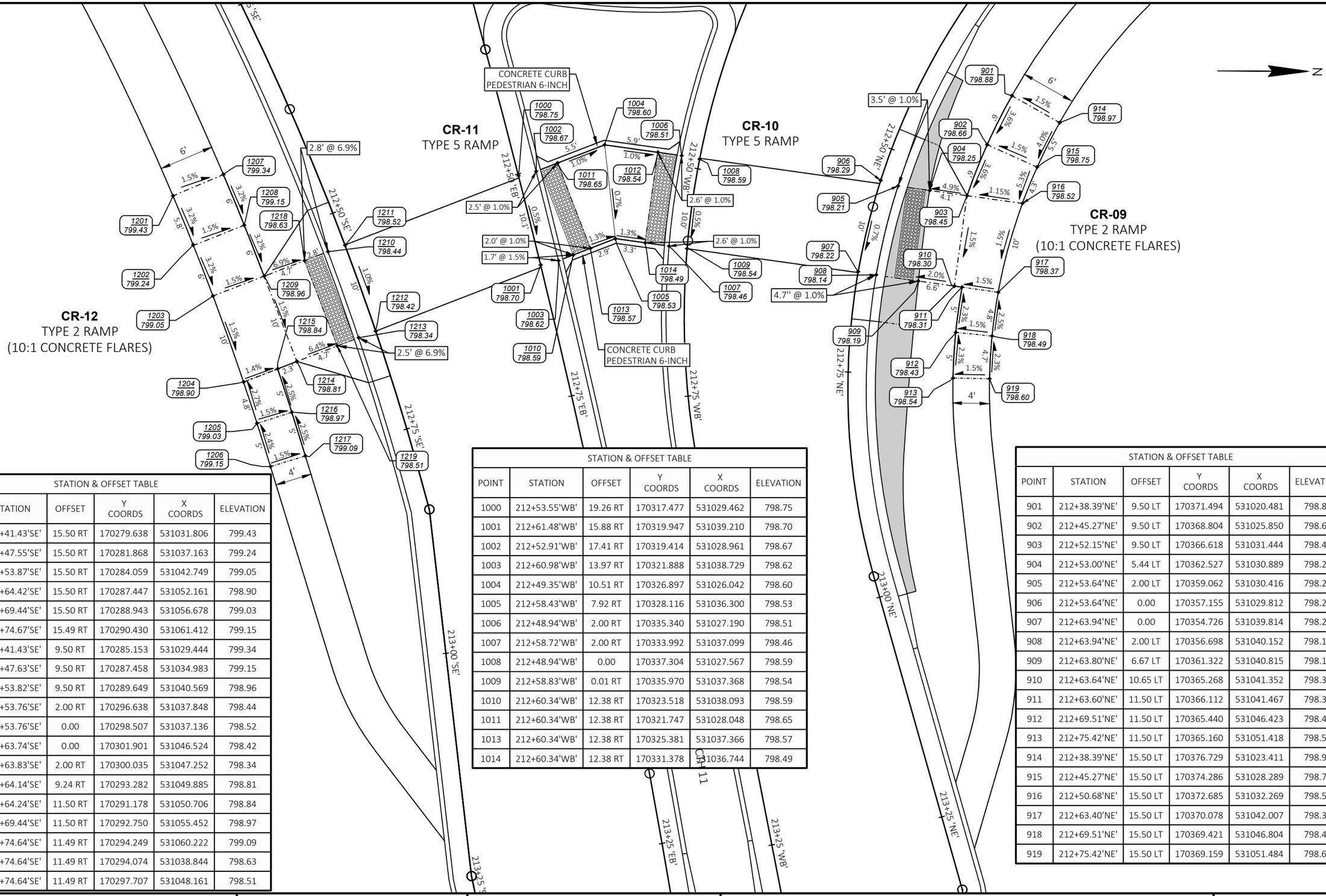
CONCRETE CURB PEDESTRIAN 6-INCH

CONCRETE CURB PEDESTRIAN 6-INCH

STH 11

PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	CURB RAMP DETAILS	SHEET 25	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\021301_CR.DWG
LAYOUT NAME - cr-1
PLOT DATE : 10/27/2023 11:46 AM
PLOT BY : KUSCHEL, LEVI
PLOT NAME :
PLOT SCALE : 1 IN:10 FT
WISDOT/CADDS SHEET 42



STATION & OFFSET TABLE

POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
1201	212+41.43'SE'	15.50 RT	170279.638	531031.806	799.43
1202	212+47.55'SE'	15.50 RT	170281.868	531037.163	799.24
1203	212+53.87'SE'	15.50 RT	170284.059	531042.749	799.05
1204	212+64.42'SE'	15.50 RT	170287.447	531052.161	798.90
1205	212+69.44'SE'	15.50 RT	170288.943	531056.678	799.03
1206	212+74.67'SE'	15.49 RT	170290.430	531061.412	799.15
1207	212+41.43'SE'	9.50 RT	170285.153	531029.444	799.34
1208	212+47.63'SE'	9.50 RT	170287.458	531034.983	799.15
1209	212+53.82'SE'	9.50 RT	170289.649	531040.569	798.96
1210	212+53.76'SE'	2.00 RT	170296.638	531037.848	798.44
1211	212+53.76'SE'	0.00	170298.507	531037.136	798.52
1212	212+63.74'SE'	0.00	170301.901	531046.524	798.42
1213	212+63.83'SE'	2.00 RT	170300.035	531047.252	798.34
1214	212+64.14'SE'	9.24 RT	170293.282	531049.885	798.81
1215	212+64.24'SE'	11.50 RT	170291.178	531050.706	798.84
1216	212+69.44'SE'	11.50 RT	170292.750	531055.452	798.97
1217	212+74.64'SE'	11.49 RT	170294.249	531060.222	799.09
1218	212+74.64'SE'	11.49 RT	170294.074	531038.844	798.63
1219	212+74.64'SE'	11.49 RT	170297.707	531048.161	798.51

STATION & OFFSET TABLE

POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
1000	212+53.55'WB'	19.26 RT	170317.477	531029.462	798.75
1001	212+61.48'WB'	15.88 RT	170319.947	531039.210	798.70
1002	212+52.91'WB'	17.41 RT	170319.414	531028.961	798.67
1003	212+60.98'WB'	13.97 RT	170321.888	531038.729	798.62
1004	212+49.35'WB'	10.51 RT	170326.897	531026.042	798.60
1005	212+58.43'WB'	7.92 RT	170328.116	531036.300	798.53
1006	212+48.94'WB'	2.00 RT	170335.340	531027.190	798.51
1007	212+58.72'WB'	2.00 RT	170333.992	531037.099	798.46
1008	212+48.94'WB'	0.00	170337.304	531027.567	798.59
1009	212+58.83'WB'	0.01 RT	170335.970	531037.368	798.54
1010	212+60.34'WB'	12.38 RT	170323.518	531038.093	798.59
1011	212+60.34'WB'	12.38 RT	170321.747	531028.048	798.65
1013	212+60.34'WB'	12.38 RT	170325.381	531037.366	798.57
1014	212+60.34'WB'	12.38 RT	170331.378	531036.744	798.49

STATION & OFFSET TABLE

POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
901	212+38.39'NE'	9.50 LT	170371.494	531020.481	798.88
902	212+45.27'NE'	9.50 LT	170368.804	531025.850	798.66
903	212+52.15'NE'	9.50 LT	170366.618	531031.444	798.45
904	212+53.00'NE'	5.44 LT	170362.527	531030.889	798.25
905	212+53.64'NE'	2.00 LT	170359.062	531030.416	798.21
906	212+53.64'NE'	0.00	170357.155	531029.812	798.29
907	212+63.94'NE'	0.00	170354.726	531039.814	798.22
908	212+63.94'NE'	2.00 LT	170356.698	531040.152	798.14
909	212+63.80'NE'	6.67 LT	170361.322	531040.815	798.19
910	212+63.64'NE'	10.65 LT	170365.268	531041.352	798.30
911	212+63.60'NE'	11.50 LT	170366.112	531041.467	798.31
912	212+69.51'NE'	11.50 LT	170365.440	531046.423	798.43
913	212+75.42'NE'	11.50 LT	170365.160	531051.418	798.54
914	212+38.39'NE'	15.50 LT	170376.729	531023.411	798.97
915	212+45.27'NE'	15.50 LT	170374.286	531028.289	798.75
916	212+50.68'NE'	15.50 LT	170372.685	531032.269	798.52
917	212+63.40'NE'	15.50 LT	170370.078	531042.007	798.37
918	212+69.51'NE'	15.50 LT	170369.421	531046.804	798.49
919	212+75.42'NE'	15.50 LT	170369.159	531051.484	798.60

PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	CURB RAMP DETAILS	SHEET 26	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\021301_CR.DWG
LAYOUT NAME - cr-3

PLOT DATE : 10/27/2023 11:46 AM PLOT BY : KUSCHEL, LEVI PLOT NAME : PLOT SCALE : 1 IN:10 FT

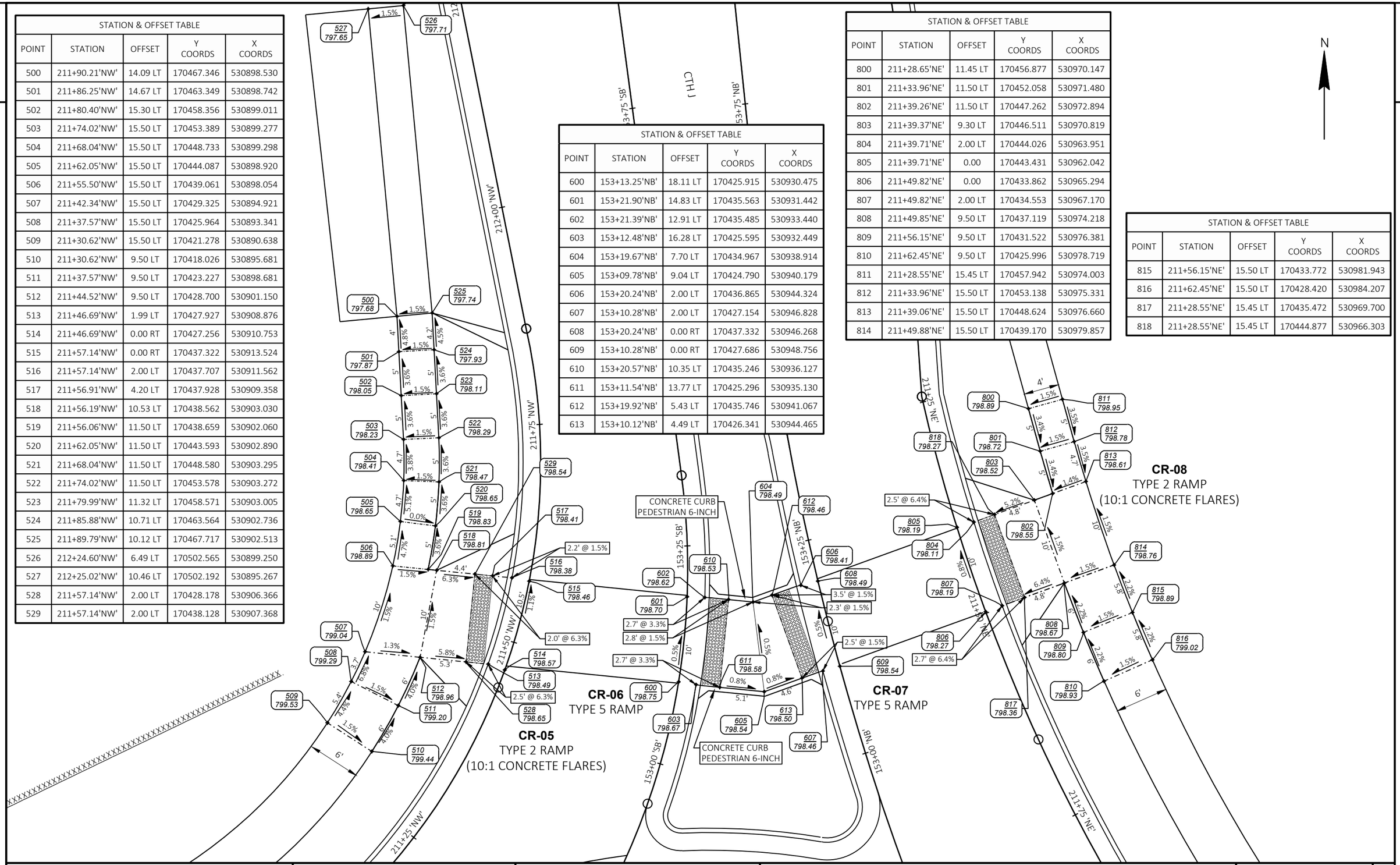
WISDOT/CADDS SHEET 42

STATION & OFFSET TABLE				
POINT	STATION	OFFSET	Y COORDS	X COORDS
500	211+90.21'NW'	14.09 LT	170467.346	530898.530
501	211+86.25'NW'	14.67 LT	170463.349	530898.742
502	211+80.40'NW'	15.30 LT	170458.356	530899.011
503	211+74.02'NW'	15.50 LT	170453.389	530899.277
504	211+68.04'NW'	15.50 LT	170448.733	530899.298
505	211+62.05'NW'	15.50 LT	170444.087	530898.920
506	211+55.50'NW'	15.50 LT	170439.061	530898.054
507	211+42.34'NW'	15.50 LT	170429.325	530894.921
508	211+37.57'NW'	15.50 LT	170425.964	530893.341
509	211+30.62'NW'	15.50 LT	170421.278	530890.638
510	211+30.62'NW'	9.50 LT	170418.026	530895.681
511	211+37.57'NW'	9.50 LT	170423.227	530898.681
512	211+44.52'NW'	9.50 LT	170428.700	530901.150
513	211+46.69'NW'	1.99 LT	170427.927	530908.876
514	211+46.69'NW'	0.00 RT	170427.256	530910.753
515	211+57.14'NW'	0.00 RT	170437.322	530913.524
516	211+57.14'NW'	2.00 LT	170437.707	530911.562
517	211+56.91'NW'	4.20 LT	170437.928	530909.358
518	211+56.19'NW'	10.53 LT	170438.562	530903.030
519	211+56.06'NW'	11.50 LT	170438.659	530902.060
520	211+62.05'NW'	11.50 LT	170443.593	530902.890
521	211+68.04'NW'	11.50 LT	170448.580	530903.295
522	211+74.02'NW'	11.50 LT	170453.578	530903.272
523	211+79.99'NW'	11.32 LT	170458.571	530903.005
524	211+85.88'NW'	10.71 LT	170463.564	530902.736
525	211+89.79'NW'	10.12 LT	170467.717	530902.513
526	212+24.60'NW'	6.49 LT	170502.565	530899.250
527	212+25.02'NW'	10.46 LT	170502.192	530895.267
528	211+57.14'NW'	2.00 LT	170428.178	530906.366
529	211+57.14'NW'	2.00 LT	170438.128	530907.368

STATION & OFFSET TABLE				
POINT	STATION	OFFSET	Y COORDS	X COORDS
600	153+13.25'NB'	18.11 LT	170425.915	530930.475
601	153+21.90'NB'	14.83 LT	170435.563	530931.442
602	153+21.39'NB'	12.91 LT	170435.485	530933.440
603	153+12.48'NB'	16.28 LT	170425.595	530932.449
604	153+19.67'NB'	7.70 LT	170434.967	530938.914
605	153+09.78'NB'	9.04 LT	170424.790	530940.179
606	153+20.24'NB'	2.00 LT	170436.865	530944.324
607	153+10.28'NB'	2.00 LT	170427.154	530946.828
608	153+20.24'NB'	0.00 RT	170437.332	530946.268
609	153+10.28'NB'	0.00 RT	170427.686	530948.756
610	153+20.57'NB'	10.35 LT	170435.246	530936.127
611	153+11.54'NB'	13.77 LT	170425.296	530935.130
612	153+19.92'NB'	5.43 LT	170435.746	530941.067
613	153+10.12'NB'	4.49 LT	170426.341	530944.465

STATION & OFFSET TABLE				
POINT	STATION	OFFSET	Y COORDS	X COORDS
800	211+28.65'NE'	11.45 LT	170456.877	530970.147
801	211+33.96'NE'	11.50 LT	170452.058	530971.480
802	211+39.26'NE'	11.50 LT	170447.262	530972.894
803	211+39.37'NE'	9.30 LT	170446.511	530970.819
804	211+39.71'NE'	2.00 LT	170444.026	530963.951
805	211+39.71'NE'	0.00	170443.431	530962.042
806	211+49.82'NE'	0.00	170433.862	530965.294
807	211+49.82'NE'	2.00 LT	170434.553	530967.170
808	211+49.85'NE'	9.50 LT	170437.119	530974.218
809	211+56.15'NE'	9.50 LT	170431.522	530976.381
810	211+62.45'NE'	9.50 LT	170425.996	530978.719
811	211+28.55'NE'	15.45 LT	170457.942	530974.003
812	211+33.96'NE'	15.50 LT	170453.138	530975.331
813	211+39.06'NE'	15.50 LT	170448.624	530976.660
814	211+49.88'NE'	15.50 LT	170439.170	530979.857

STATION & OFFSET TABLE				
POINT	STATION	OFFSET	Y COORDS	X COORDS
815	211+56.15'NE'	15.50 LT	170433.772	530981.943
816	211+62.45'NE'	15.50 LT	170428.420	530984.207
817	211+28.55'NE'	15.45 LT	170435.472	530969.700
818	211+28.55'NE'	15.45 LT	170444.877	530966.303



2

2

N

STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
1600	211+48.83'SW'	15.50 RT	170240.213	530878.404	800.11
1601	211+55.13'SW'	15.50 RT	170234.741	530880.361	799.91
1602	211+61.26'SW'	15.50 RT	170229.361	530882.099	799.72
1603	211+72.10'SW'	15.50 RT	170219.723	530884.767	799.57
1604	211+76.23'SW'	15.50 RT	170216.017	530885.645	799.74
1605	211+80.47'SW'	15.50 RT	170212.189	530886.467	799.90
1606	211+48.83'SW'	9.50 RT	170242.322	530884.021	800.02
1607	211+55.13'SW'	9.50 RT	170236.673	530886.042	799.82
1608	211+61.43'SW'	9.50 RT	170230.962	530887.884	799.63
1609	211+61.61'SW'	2.00 RT	170232.975	530895.111	799.14
1610	211+61.68'SW'	0.00	170233.497	530897.043	799.22
1611	211+71.74'SW'	0.00	170223.798	530899.725	799.12
1612	211+71.74'SW'	2.00 RT	170223.313	530897.785	799.04
1613	211+71.93'SW'	9.50 RT	170221.324	530890.551	799.48
1614	211+71.98'SW'	11.50 RT	170220.791	530888.623	799.51
1615	211+76.23'SW'	11.50 RT	170216.898	530889.546	799.68
1616	211+80.47'SW'	11.50 RT	170212.988	530890.386	799.84

STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
1400	151+04.74'NB'	15.05 LT	170228.142	530914.631	799.37
1401	151+12.42'NB'	18.95 LT	170237.813	530912.394	799.42
1402	151+13.17'NB'	17.16 LT	170238.296	530914.335	799.34
1403	151+05.33'NB'	13.17 LT	170228.561	530916.587	799.29
1404	151+16.58'NB'	10.21 LT	170240.413	530922.031	799.27
1405	151+07.48'NB'	7.74 LT	170230.115	530922.315	799.23
1406	151+16.95'NB'	2.00 LT	170238.633	530930.057	799.19
1407	151+07.22'NB'	2.00 LT	170228.916	530927.935	799.13
1408	151+16.95'NB'	0.00	170238.102	530931.985	799.27
1409	151+07.22'NB'	0.00	170228.596	530929.910	799.21
1410	151+07.22'NB'	0.00	170238.950	530916.748	799.32
1411	151+07.22'NB'	0.00	170229.311	530919.410	799.26
1412	151+07.22'NB'	0.00	170229.452	530925.353	799.16
1413	151+07.22'NB'	0.00	170239.221	530927.491	799.22

STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
1300	211+58.78'SE'	9.50 RT	170242.506	530966.264	799.80
1301	211+51.97'SE'	9.50 RT	170237.393	530963.124	799.60
1302	211+45.15'SE'	9.50 RT	170232.031	530960.429	799.41
1303	211+43.63'SE'	2.00 RT	170233.750	530952.994	798.95
1304	211+43.63'SE'	0.00	170234.537	530951.155	799.03
1305	211+33.22'SE'	0.00	170224.726	530947.695	798.96
1306	211+33.22'SE'	2.00 RT	170224.185	530949.621	798.88
1307	211+33.77'SE'	10.51 RT	170222.338	530957.945	799.25
1308	211+33.84'SE'	11.50 RT	170222.127	530958.912	799.27
1309	211+28.00'SE'	11.50 RT	170217.277	530957.699	799.46
1310	211+22.16'SE'	11.50 RT	170212.351	530956.842	799.65
1311	211+58.78'SE'	15.50 RT	170239.151	530971.239	799.89
1312	211+51.97'SE'	15.50 RT	170234.473	530968.365	799.69
1313	211+46.73'SE'	15.50 RT	170230.721	530966.433	799.48
1314	211+34.17'SE'	15.50 RT	170221.272	530962.830	799.33
1315	211+28.00'SE'	15.50 RT	170216.448	530961.612	799.52
1316	211+22.16'SE'	15.50 RT	170211.810	530960.805	799.71
1317	211+33.30'SE'	3.74 RT	170223.785	530951.316	798.91

CONCRETE CURB PEDESTRIAN 6-INCH

CONCRETE CURB PEDESTRIAN 6-INCH

CTH J

CR-16
TYPE 2 RAMP
(10:1 CONCRETE FLARES)

CR-15
TYPE 5 RAMP

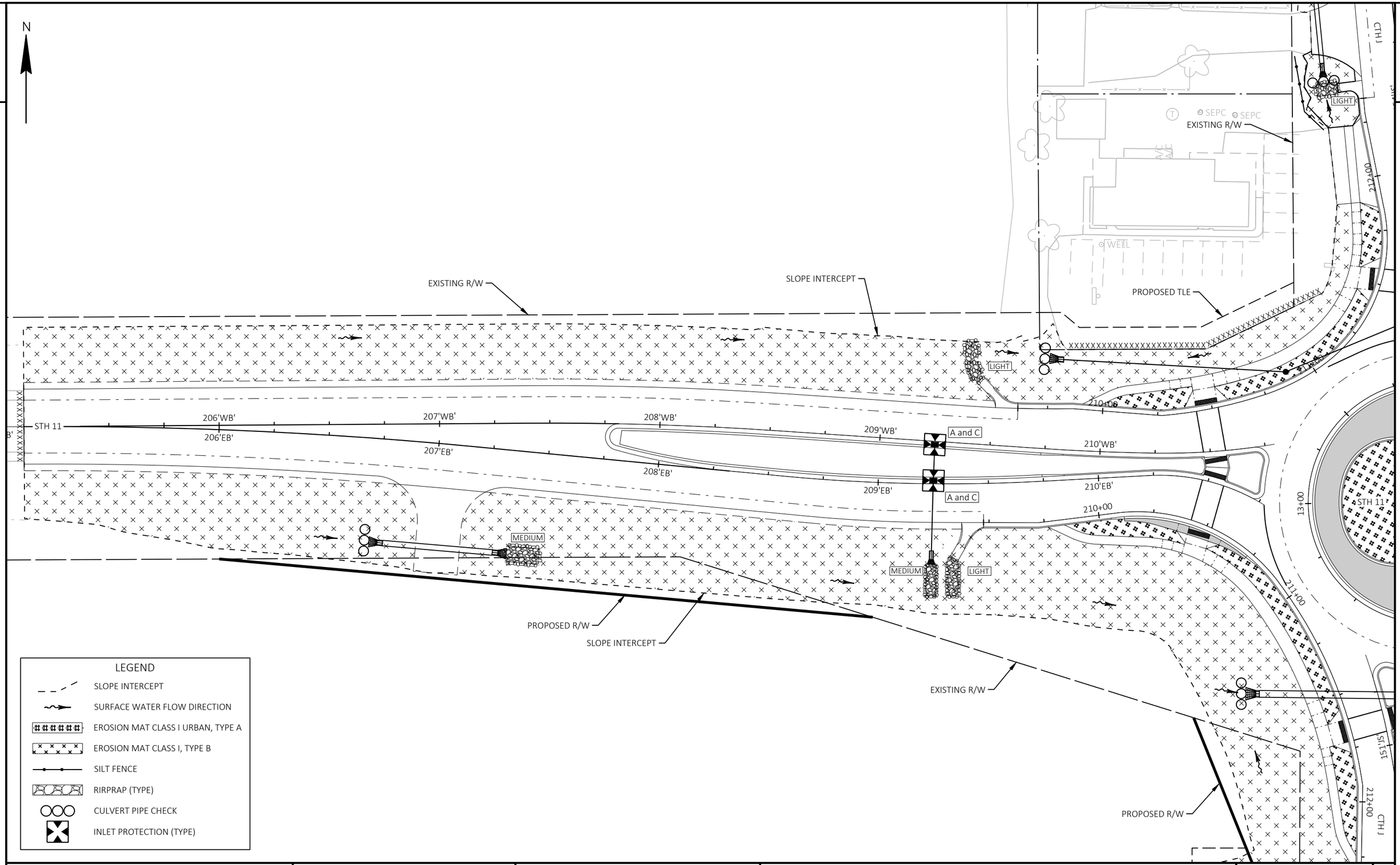
CR-14
TYPE 5 RAMP

CR-13
TYPE 2 RAMP
(10:1 CONCRETE FLARES)

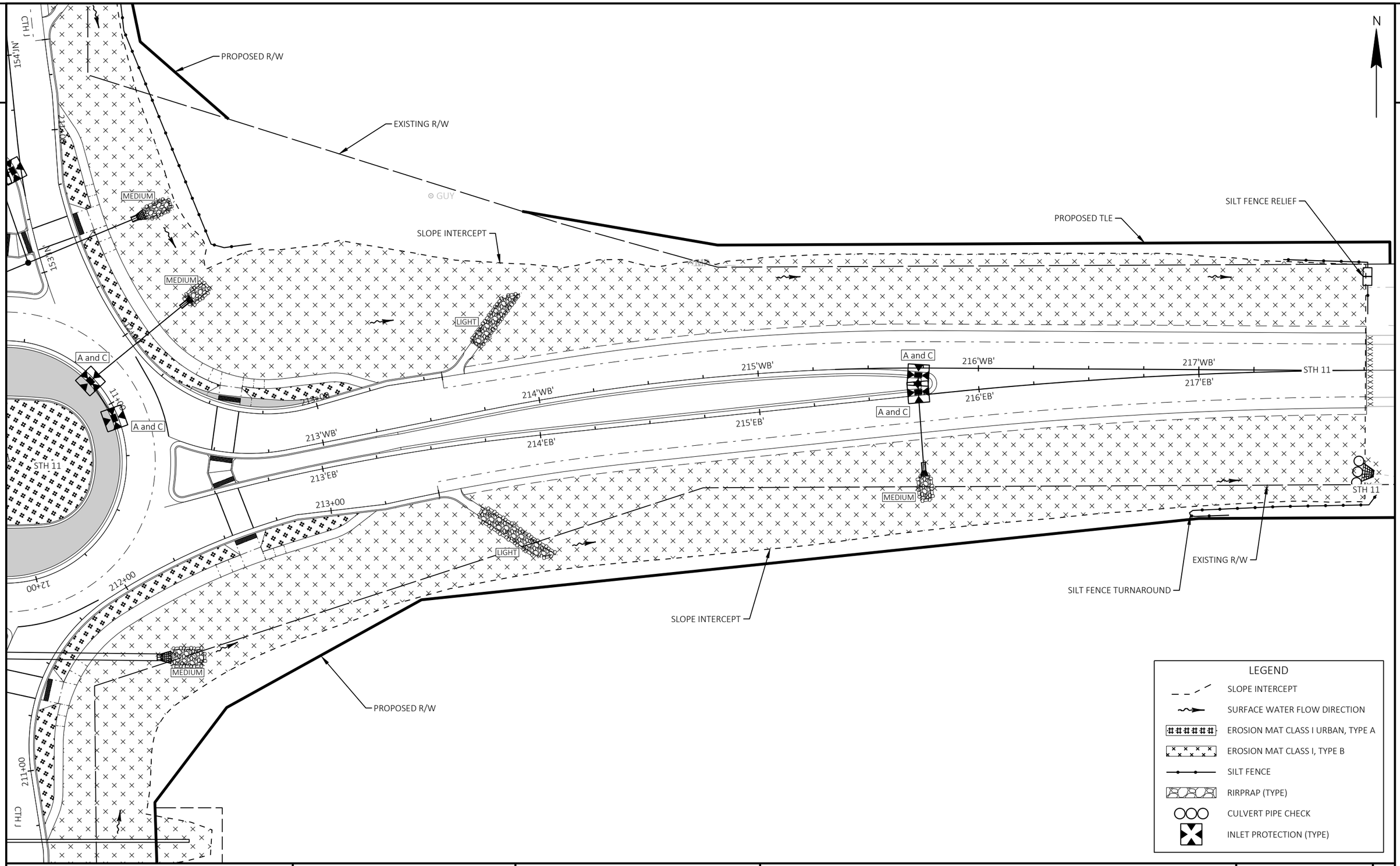
PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	CURB RAMP DETAILS	SHEET 28	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\021301_CR.DWG
LAYOUT NAME - cr-4

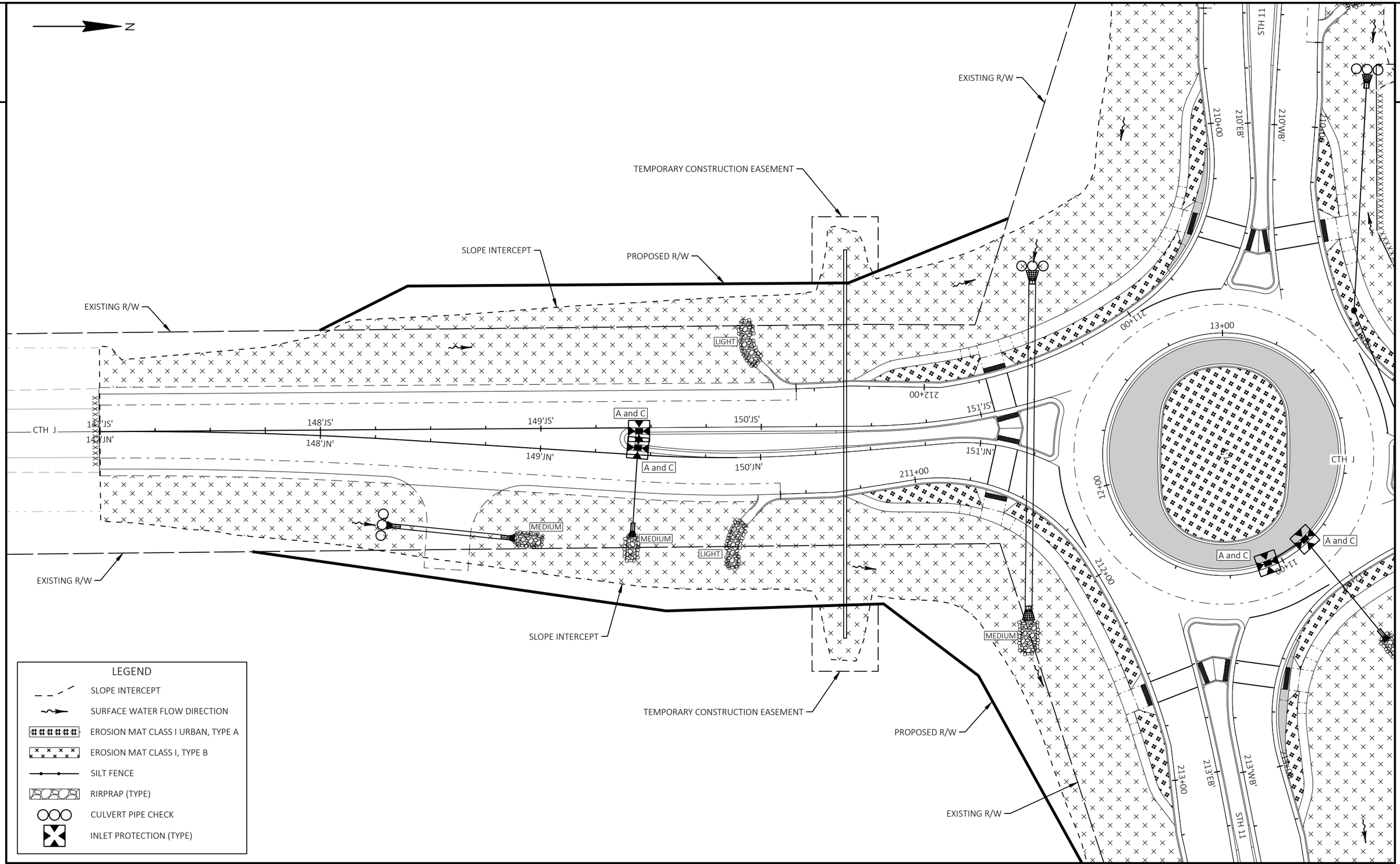
PLOT DATE : 10/27/2023 11:46 AM PLOT BY : KUSCHEL, LEVI PLOT NAME : PLOT SCALE : 1 IN:10 FT



PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	EROSION CONTROL	SHEET 29	E



PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	EROSION CONTROL	SHEET 30	E



PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	EROSION CONTROL	SHEET 31	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\022001-EC.DWG
LAYOUT NAME - 03

PLOT DATE : 10/27/2023 12:21 PM PLOT BY : KUSCHEL, LEVI PLOT NAME : PLOT SCALE : 1 IN:40 FT

2

2

N

EXISTING R/W

EXISTING PROPERTY LINE (TYP)

PROPOSED TLE

~2" DIA. PVC DRAIN PIPE ORIGINATING FROM RESTAURANT TO REMAIN. DO NOT DISTURB.

SLOPE INTERCEPT

EXISTING R/W

PROPOSED R/W

SLOPE INTERCEPT

EXISTING R/W

STH 11

CTH J

LIGHT

MEDIUM

A and C

WELL

SEPC

A/C

VP

210+00

210'EB'

210'WB'

211+00

212+00

213+00

213'EB'

213'WB'

12+00

13+00

153'JS'

154'JS'

155'JS'

156'JS'

157'JN'

156'JN'

155'JN'

154'JN'

153'JN'

LEGEND

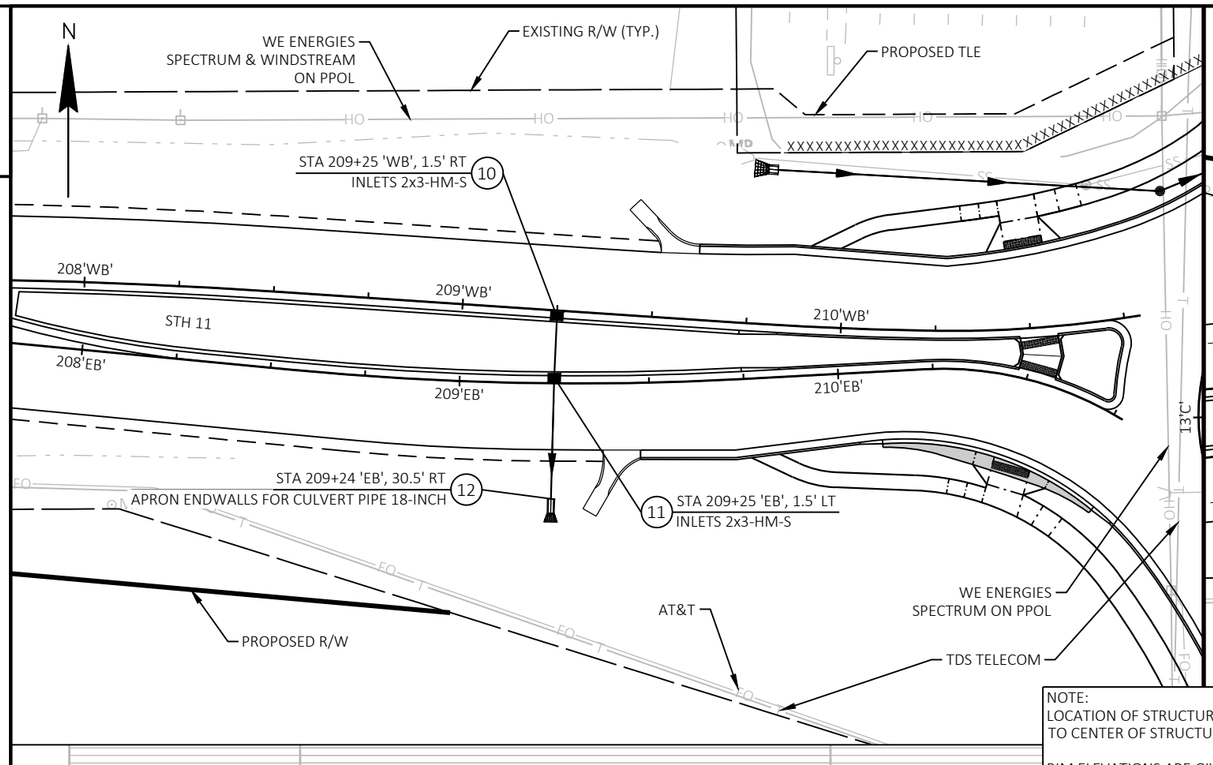
- SLOPE INTERCEPT
- SURFACE WATER FLOW DIRECTION
- EROSION MAT CLASS I URBAN, TYPE A
- EROSION MAT CLASS I, TYPE B
- SILT FENCE
- RIRPRAP (TYPE)
- CULVERT PIPE CHECK
- INLET PROTECTION (TYPE)

PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	EROSION CONTROL	SHEET 32	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\022001-EC.DWG
LAYOUT NAME - 04

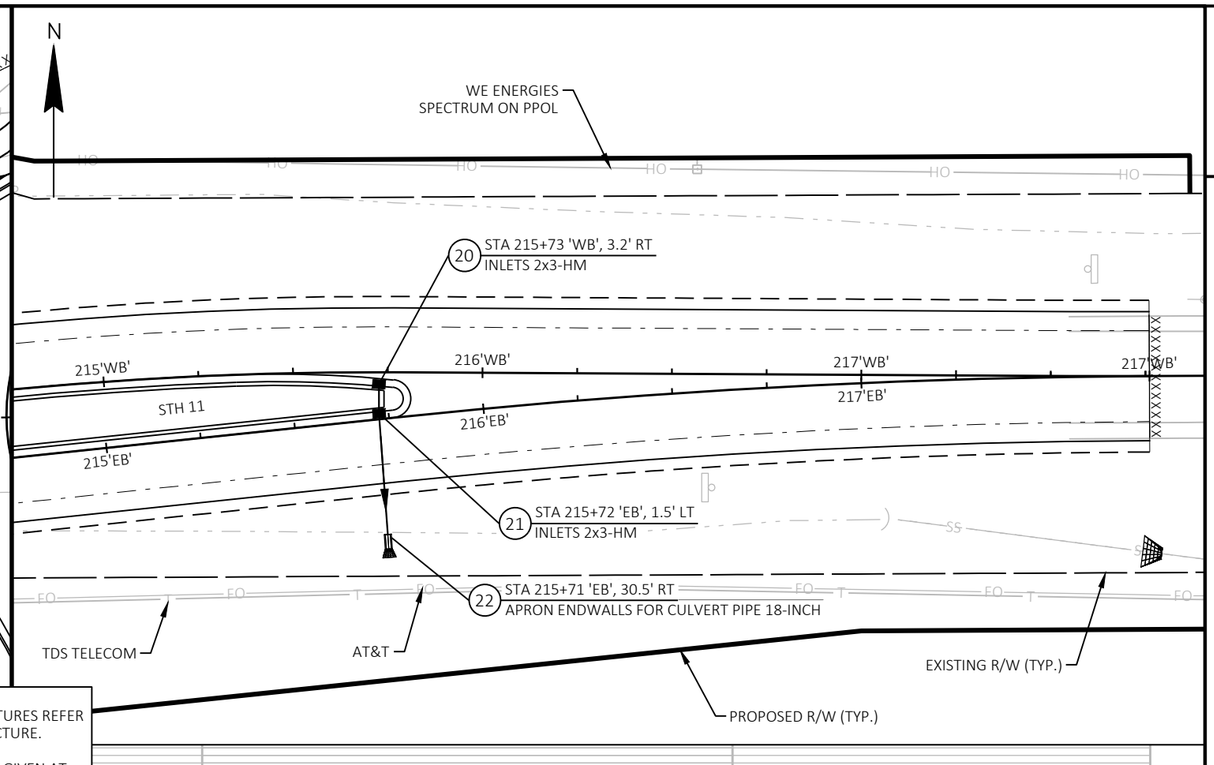
PLOT DATE : 10/27/2023 12:21 PM PLOT BY : KUSCHEL, LEVI PLOT NAME : PLOT SCALE : 1 IN:40 FT

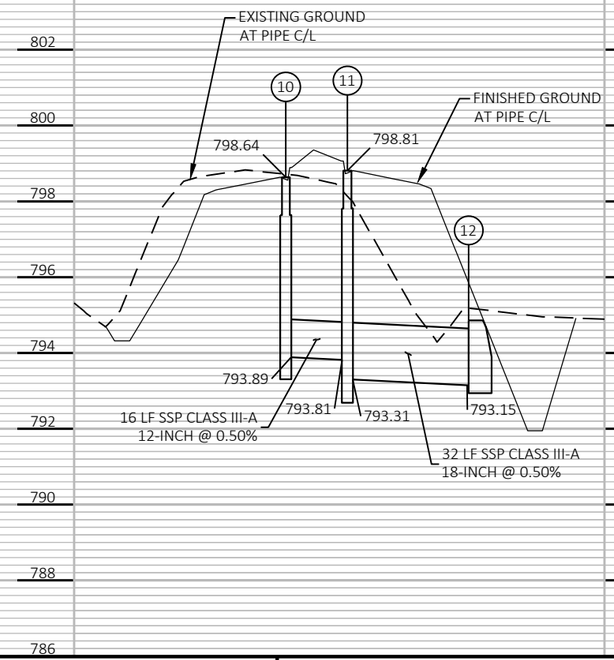
WISDOT/CADDS SHEET 42

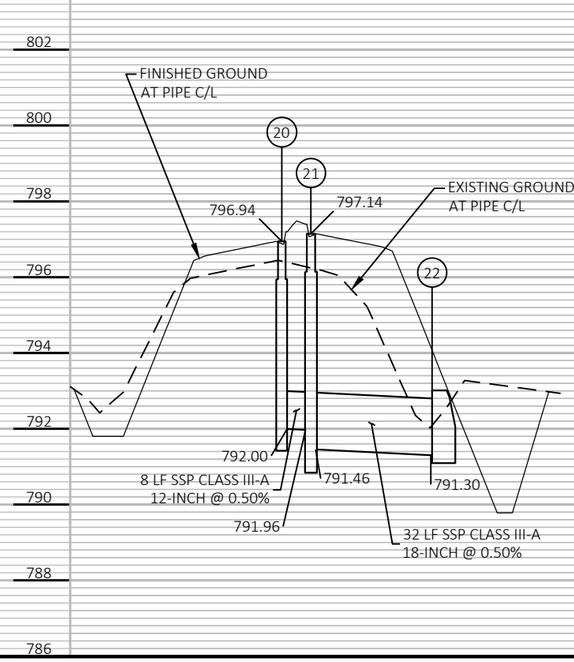


NOTE:
LOCATION OF STRUCTURES REFER TO CENTER OF STRUCTURE.

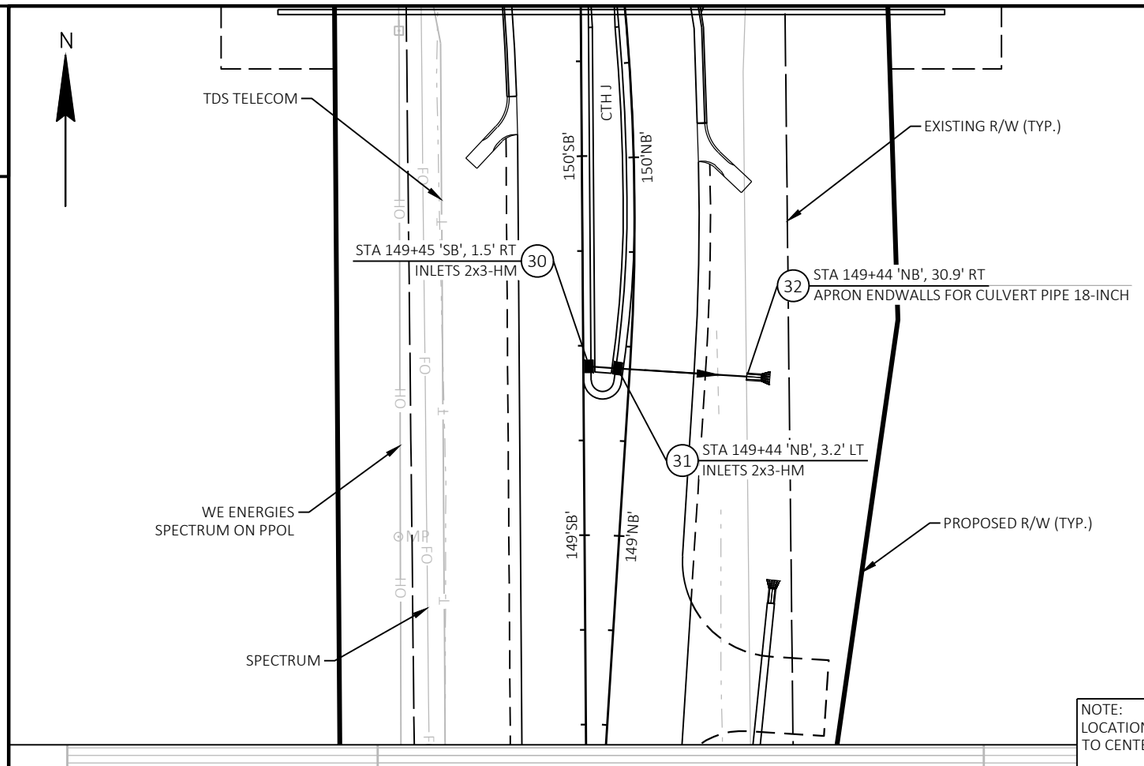
RIM ELEVATIONS ARE GIVEN AT THE FLANGE LINE OF INLET OR AT CENTER OF MANHOLE.

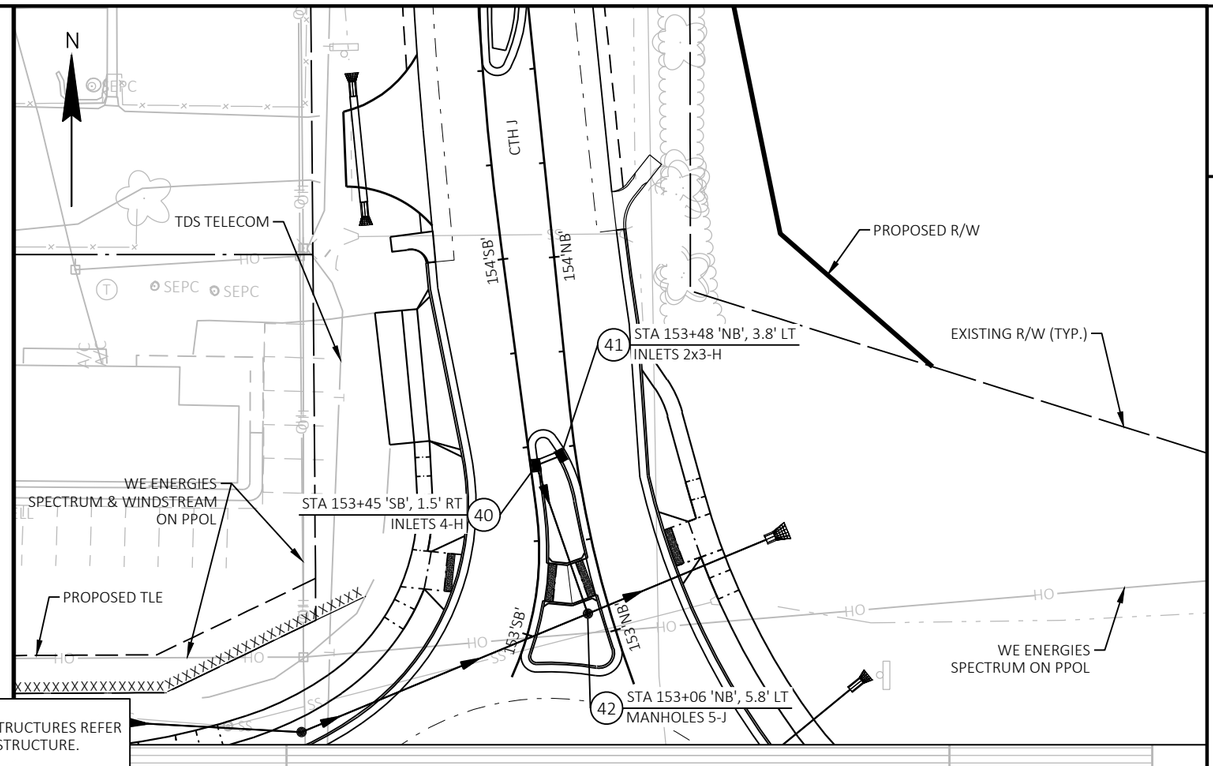






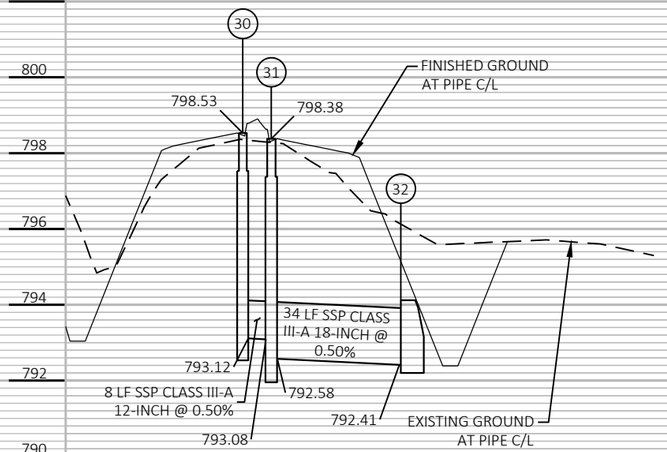
PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	STORM SEWER	SHEET 33	E

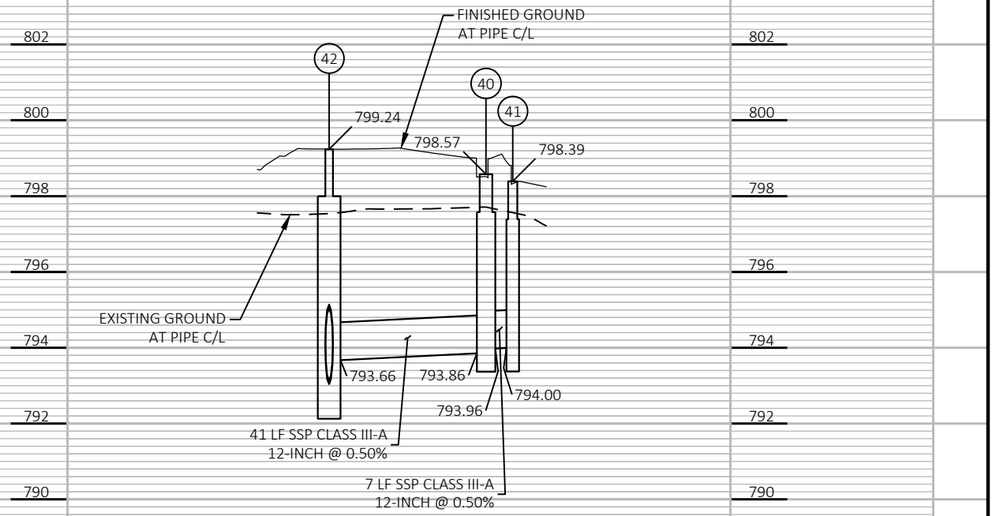




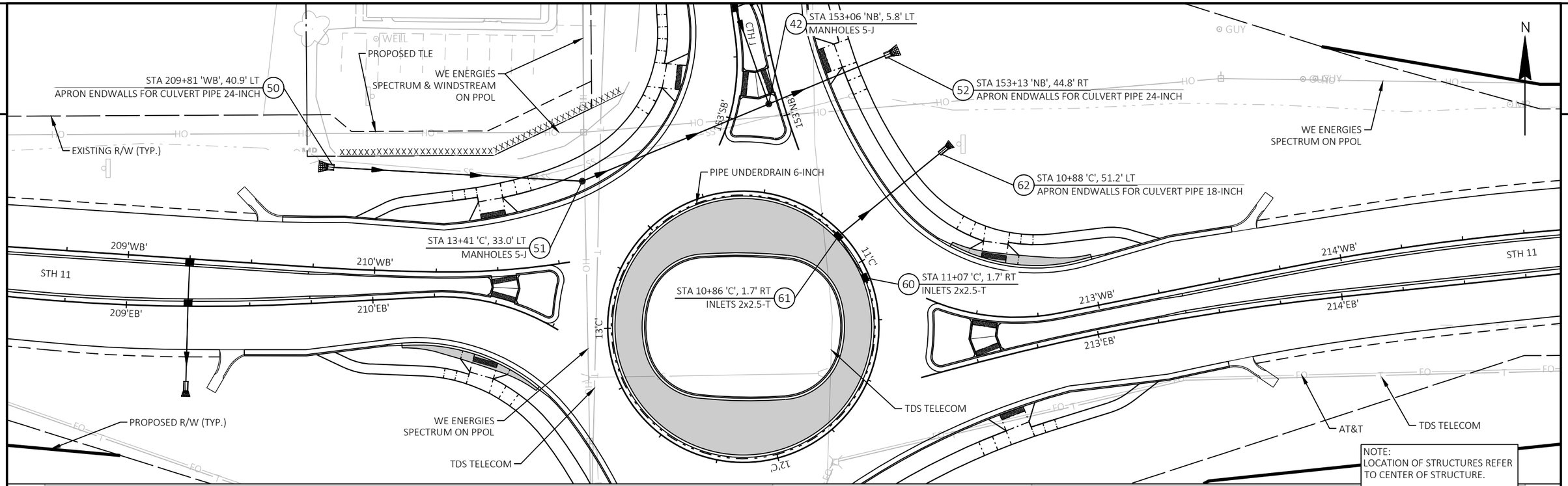
NOTE:
LOCATION OF STRUCTURES REFER TO CENTER OF STRUCTURE.

RIM ELEVATIONS ARE GIVEN AT THE FLANGE LINE OF INLET OR AT CENTER OF MANHOLE.





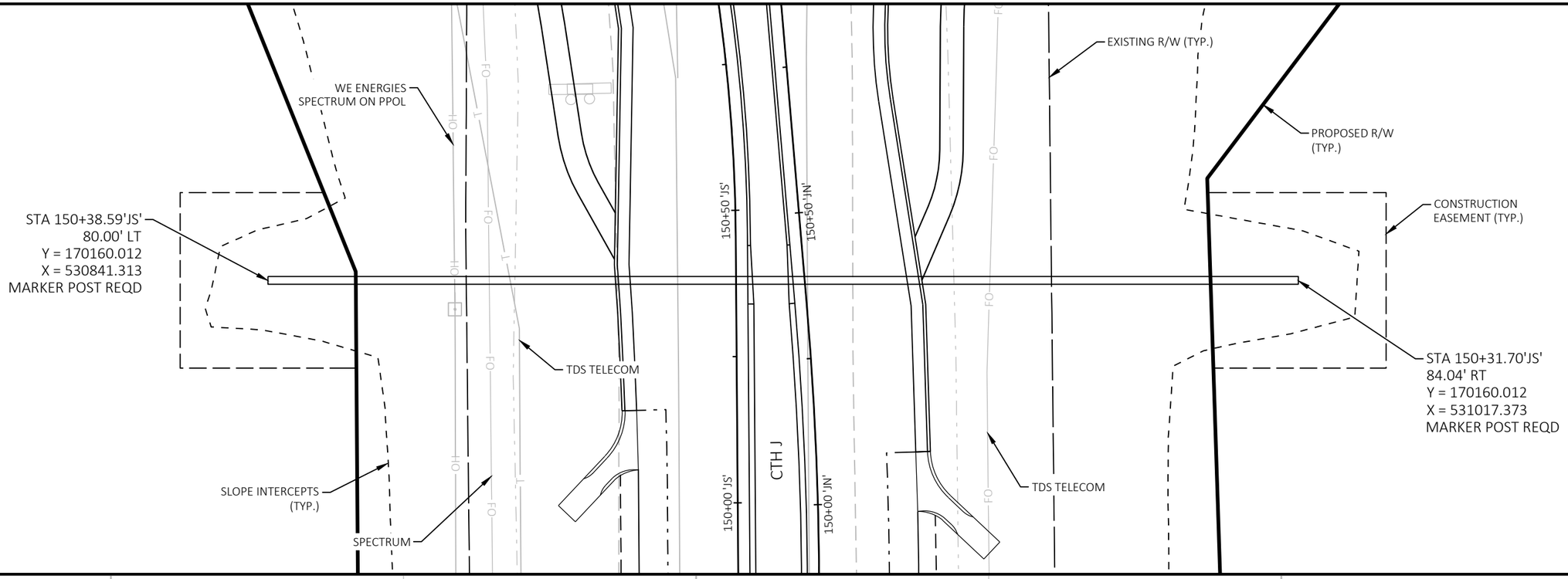
PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	STORM SEWER	SHEET 34	E

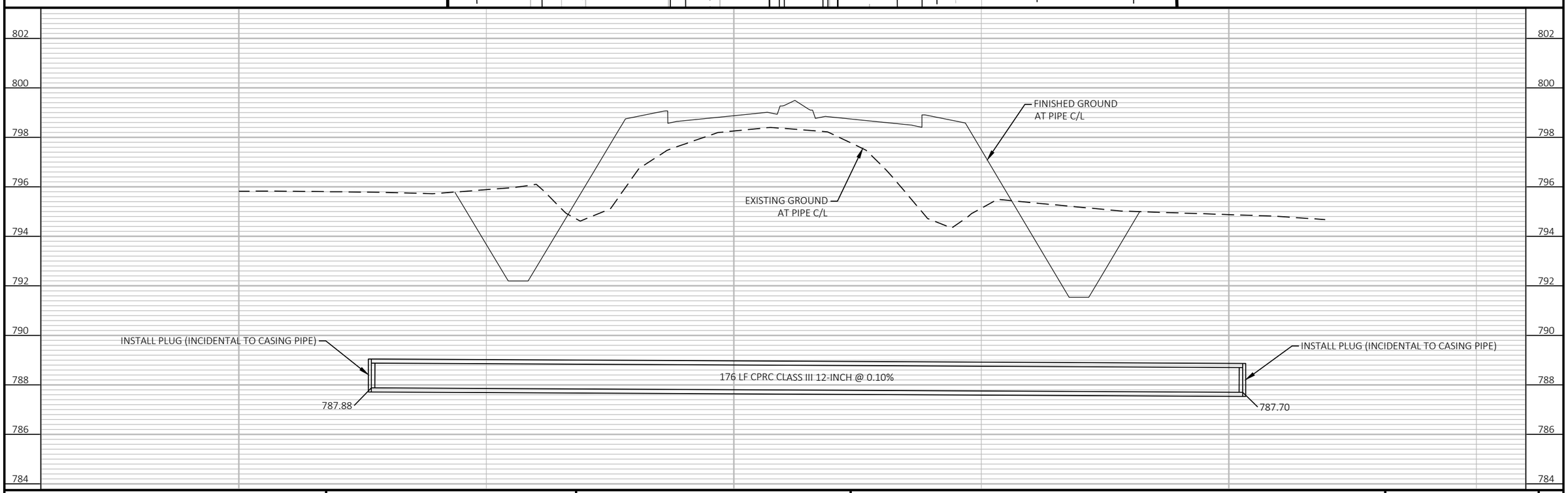


PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	STORM SEWER	SHEET 35	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\022501-SS.DWG
LAYOUT NAME - 03

PLOT DATE : 1/19/2024 8:32 AM PLOT BY : MUENCH, DOUGLAS PLOT NAME : PLOT SCALE : 1 IN:40 FT





PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	STORM SEWER: CASING PIPE	SHEET 36	E

2

2

N

HO HO HO HO HO HO HO HO HO HO HO HO HO HO

FO FO FO FO FO FO FO FO FO FO FO FO FO

204'EB'

205'EB'

206'WB'

206'EB'

207'WB'

207'EB'

208'WB'

208'EB'

209'WB'

209'EB'

MATCHLINE STA 209+25 EB

STA 206+76, 24' RT
LED-A-5(T)-6-BCJ1

#10 B,N,GND

STA 208+62, 23' RT
LED-A-5(T)-6-ACJ1

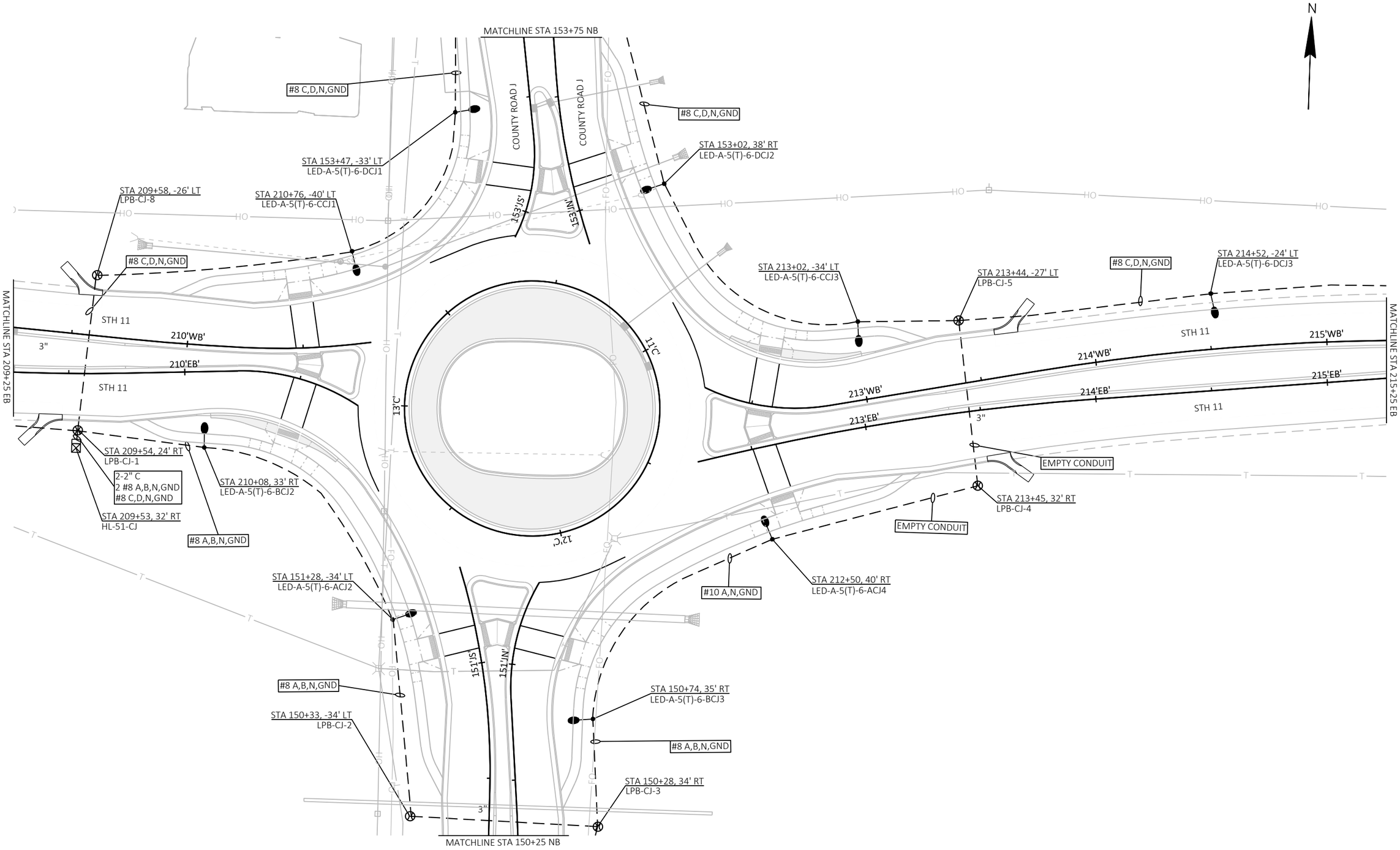
#8 A,B,N,GND

PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	LIGHTING PLANS - PROPOSED	SHEET 37	E

FILE NAME : V:\PROJECTS\22261.01\WISDOT PROJECT\STH 11\04_CADD\CADD_SHEETS\023601_LP.DWG
LAYOUT NAME - LP1

PLOT DATE : 7/12/2023 10:39 AM PLOT BY : ALEXANDER UNDERWOOD PLOT NAME : PLOT SCALE : 1 IN:40 FT

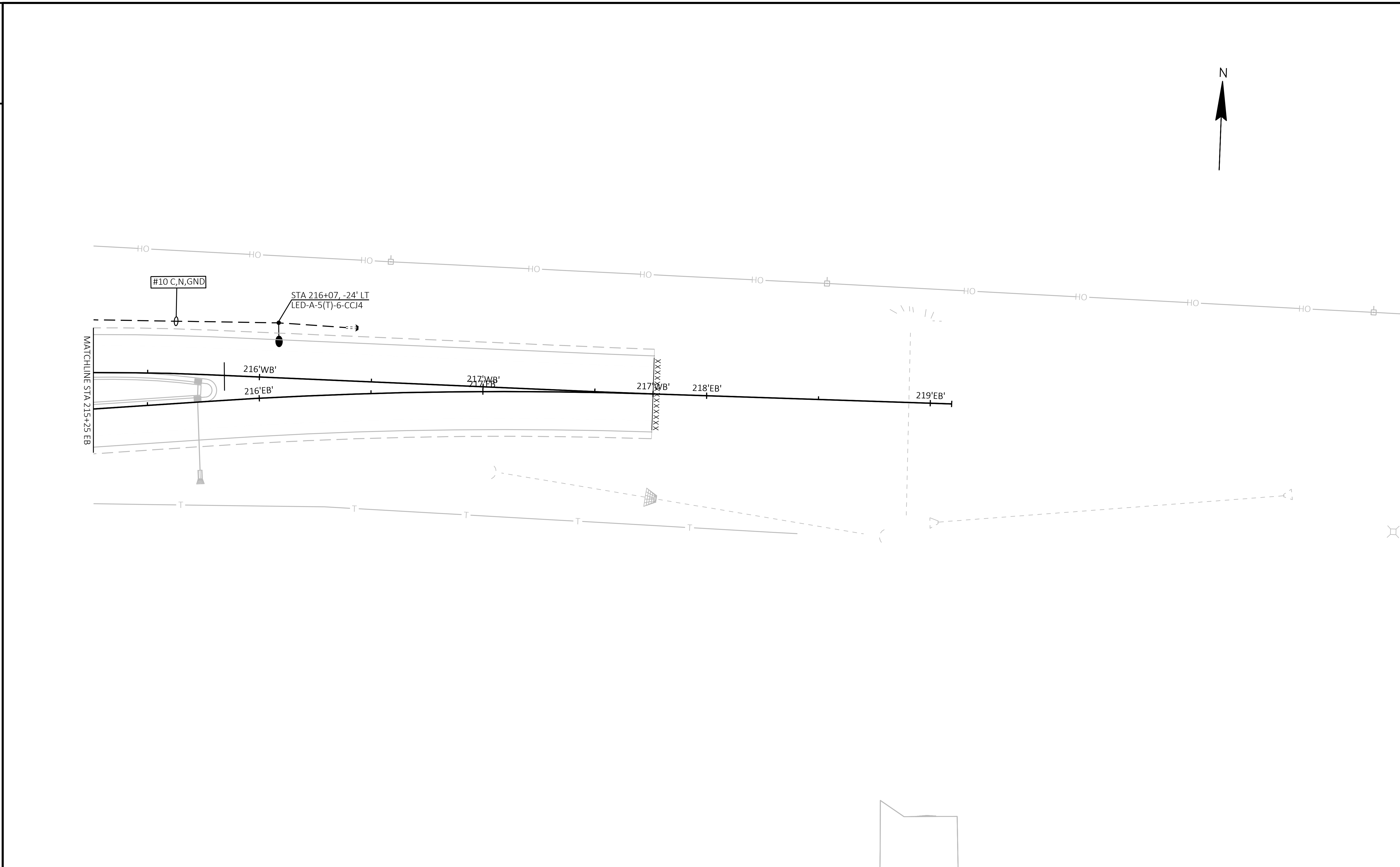
2



PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	LIGHTING PLANS - PROPOSED	SHEET 38	E

FILE NAME : V:\PROJECTS\22261.01\WISDOT PROJECT\STH 11\04_CADD\CADD_SHEETS\023601_LP.DWG
LAYOUT NAME - LP2

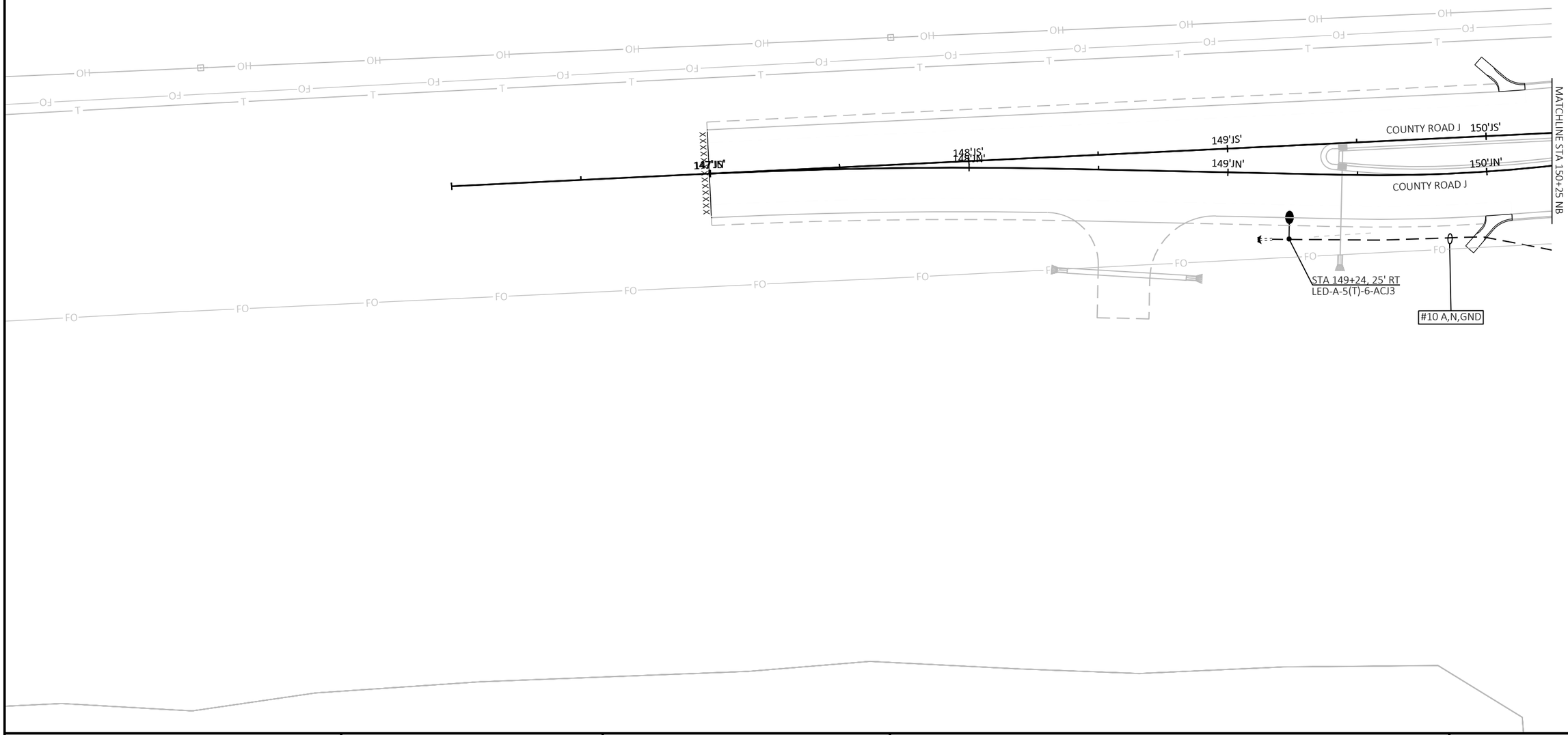
PLOT DATE : 7/14/2023 10:06 AM PLOT BY : ALEXANDER UNDERWOOD PLOT NAME : PLOT SCALE : 1 IN:40 FT



PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	LIGHTING PLANS - PROPOSED	SHEET 39	E

2

N



PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	LIGHTING PLANS - PROPOSED	SHEET 40	E

2

N

STA 153+99, -27' LT
LPB-CJ-7

STA 155+17, -24' LT
LED-A-5(T)-6-CCJ2

#10 C,N,GND

#8 C,D,N,GND

MATCHLINE STA 153+75 NB

154'JS'

3"

154'JN'

155'JS'

155'JN'

156'JS'

156'JN'

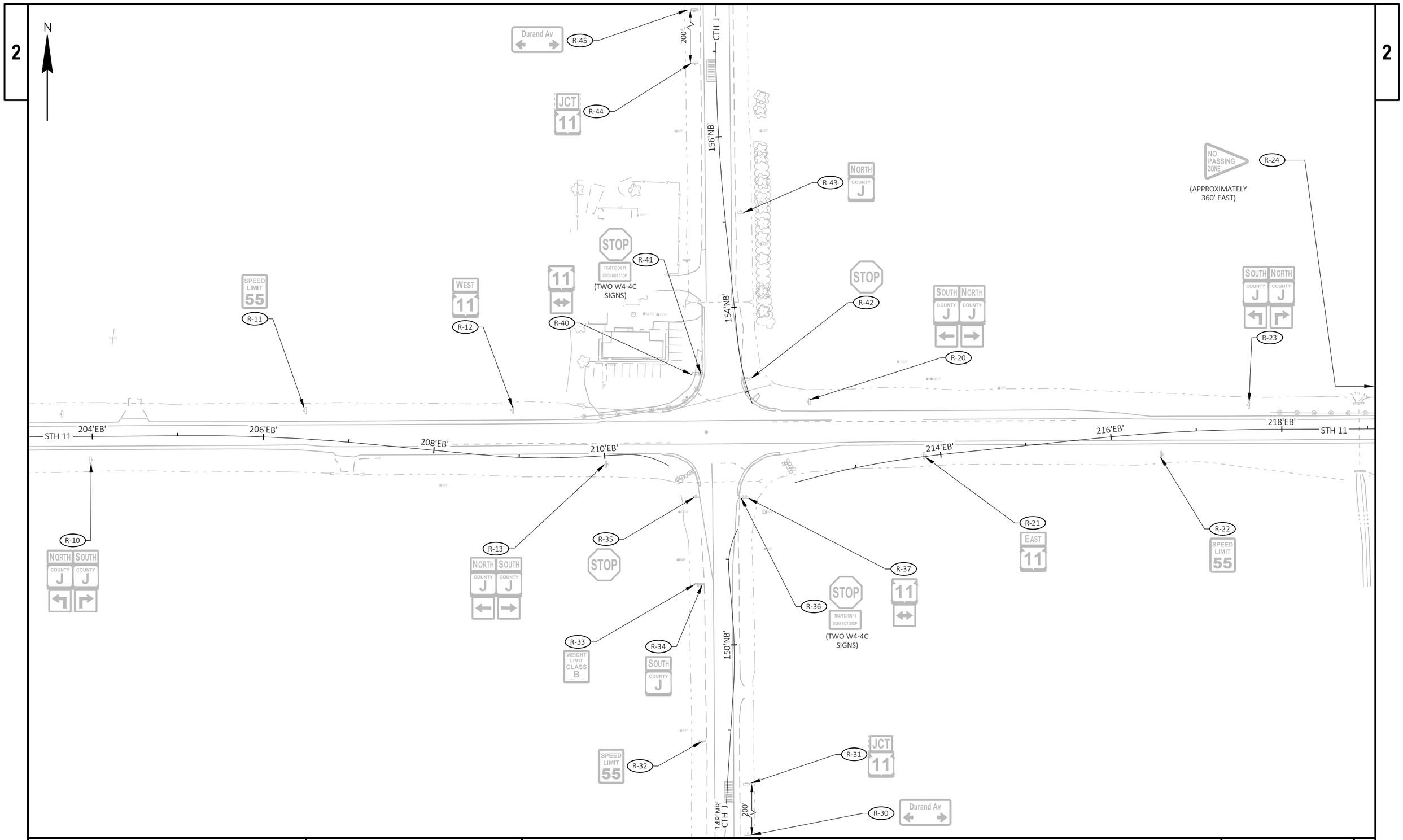
157'JN'

158'JN'

159'JN'

STA 153+97, 27' RT
LPB-CJ-6

PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	LIGHTING PLANS - PROPOSED	SHEET 41	E



PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	SIGN REMOVAL PLAN	SHEET 42	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\023200-PS (SIGN REMOVAL).DWG
LAYOUT NAME - 01

PLOT DATE : 10/23/2023 1:54 PM PLOT BY : KUSCHEL, LEVI PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 42

2

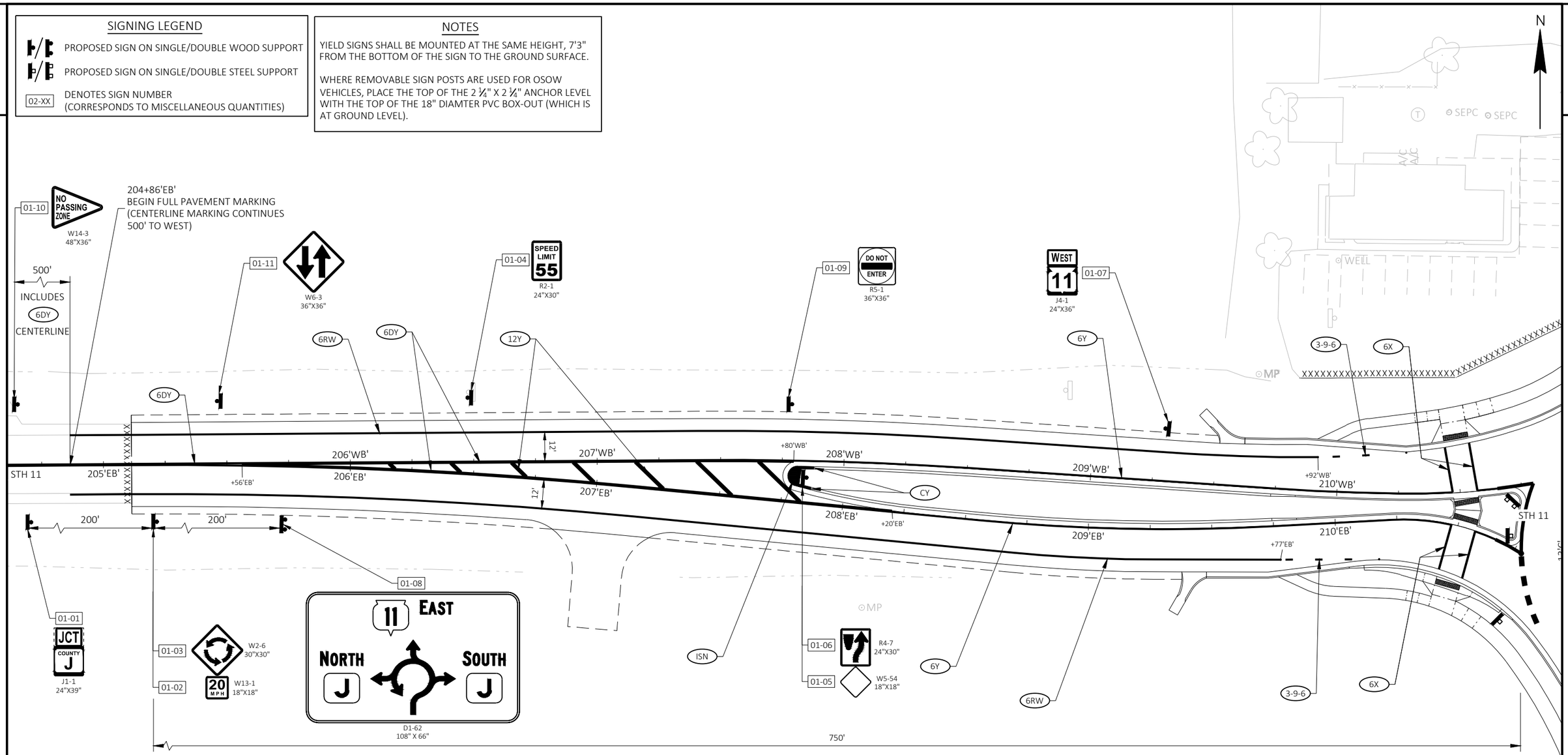
SIGNING LEGEND

- PROPOSED SIGN ON SINGLE/DOUBLE WOOD SUPPORT
- PROPOSED SIGN ON SINGLE/DOUBLE STEEL SUPPORT
- 02-XX DENOTES SIGN NUMBER (CORRESPONDS TO MISCELLANEOUS QUANTITIES)

NOTES

YIELD SIGNS SHALL BE MOUNTED AT THE SAME HEIGHT, 7'3" FROM THE BOTTOM OF THE SIGN TO THE GROUND SURFACE.

WHERE REMOVABLE SIGN POSTS ARE USED FOR OSOW VEHICLES, PLACE THE TOP OF THE 2 ¼" X 2 ¼" ANCHOR LEVEL WITH THE TOP OF THE 18" DIAMTER PVC BOX-OUT (WHICH IS AT GROUND LEVEL).



MARKING LEGEND

- 4W MARKING LINE EPOXY 4-INCH (WHITE)
- 6X MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)
- 6RW MARKING LINE GROOVED WET REFLECTIVE EPOXY 6-INCH (WHITE)
- 4Y MARKING LINE EPOXY 4-INCH (YELLOW)
- 6Y MARKING LINE EPOXY 6-INCH (YELLOW)
- 4DY MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW)
- 6DY MARKING LINE EPOXY 6-INCH (DOUBLE YELLOW)
- 3-9-4 MARKING LINE EPOXY 4-INCH (WHITE 3' DASH, 9' SKIP)
- 3-9-6 MARKING LINE GROOVED WET REFLECTIVE EPOXY 6-INCH (WHITE 3' DASH, 9' SKIP)
- 10W MARKING LINE EPOXY 10-INCH (WHITE)
- 12Y MARKING DIAGONAL EPOXY 12-INCH (YELLOW)
- 2-2 MARKING DOTTED EXTENSION EPOXY 18-INCH (WHITE 2' DASH, 2' SKIP)
- CY MARKING CURB EPOXY (YELLOW)
- ISN MARKING ISLAND NOSE EPOXY (YELLOW)
- SD-Y MARKING LINE EPOXY 4-INCH (YELLOW 12.5' DASH, 37.5' SKIP)

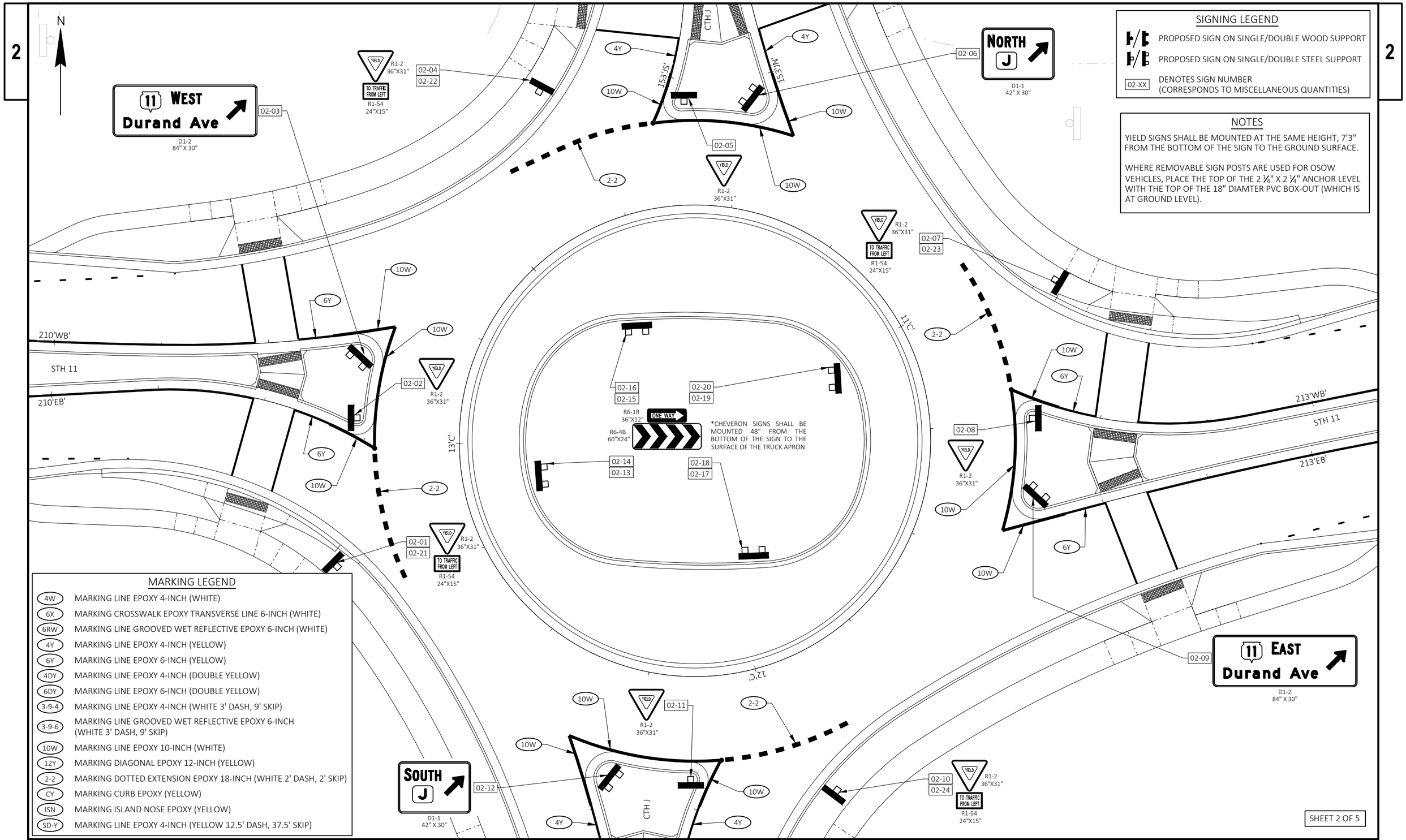
SHEET 1 OF 5

PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	PERMANENT SIGNING AND PAVEMENT MARKING	SHEET 43	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\023201PS-PM.DWG
LAYOUT NAME - 01

PLOT DATE : 10/27/2023 12:43 PM PLOT BY : KUSCHEL, LEVI PLOT NAME : PLOT SCALE : 1 IN:40 FT

WISDOT/CADDS SHEET 42



PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	PERMANENT SIGNING AND PAVEMENT MARKING	SHEET 44	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\023201PS-PM.DWG
LAYOUT NAME - 02
PLOT DATE : 10/27/2023 12:43 PM PLOT BY : KUSCHEL, LEVI PLOT NAME : PLOT SCALE : 1 IN:20 FT

2

N

750'

11 WEST
SOUTH J NORTH J
D1-62
108" X 66"

W2-6
30"X30"
03-06
20 MPH
W13-1
18"X18"
03-05
03-08

JCT COUNTY J
J1-1
24"X39"
03-07

200'
200'

6X
3-9-6
6Y
6RW
CY
ISN
12Y
6DY

GUY
MP

STH 11

213'WB'
214'WB'
215'WB'
+34'WB'
+50'WB'
216'WB'
217'WB'
+43'WB'
217'EB'
216'EB'
215'EB'
214'EB'
213'EB'
+25'EB'
218'EB'
12'

6DY
6DY

218+01'EB'
END FULL PAVEMENT MARKING
(CENTERLINE MARKING CONTINUES
500' TO EAST)

03-12
NO PASSING ZONE
W14-3

03-03
R4-7
24"X30"
03-02
W5-54
18"X18"
03-09
DO NOT ENTER
R5-1
30"X30"

SPEED LIMIT 55
R2-1
24"X30"
03-04

03-11
W6-3
36"X36"

500'
(INCLUDES
6DY
CENTERLINE)

NO PASSING ZONE
W14-3
48"X36"
03-10

03-01
EAST 11
J4-1
24"X36"

6X
3-9-6
6Y
6RW

MARKING LEGEND

4W	MARKING LINE EPOXY 4-INCH (WHITE)
6X	MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)
6RW	MARKING LINE GROOVED WET REFLECTIVE EPOXY 6-INCH (WHITE)
4Y	MARKING LINE EPOXY 4-INCH (YELLOW)
6Y	MARKING LINE EPOXY 6-INCH (YELLOW)
4DY	MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW)
6DY	MARKING LINE EPOXY 6-INCH (DOUBLE YELLOW)
3-9-4	MARKING LINE EPOXY 4-INCH (WHITE 3' DASH, 9' SKIP)
3-9-6	MARKING LINE GROOVED WET REFLECTIVE EPOXY 6-INCH (WHITE 3' DASH, 9' SKIP)
10W	MARKING LINE EPOXY 10-INCH (WHITE)
12Y	MARKING DIAGONAL EPOXY 12-INCH (YELLOW)
2-2	MARKING DOTTED EXTENSION EPOXY 18-INCH (WHITE 2' DASH, 2' SKIP)
CY	MARKING CURB EPOXY (YELLOW)
ISN	MARKING ISLAND NOSE EPOXY (YELLOW)
SD-Y	MARKING LINE EPOXY 4-INCH (YELLOW 12.5' DASH, 37.5' SKIP)

NOTES

YIELD SIGNS SHALL BE MOUNTED AT THE SAME HEIGHT, 7'3" FROM THE BOTTOM OF THE SIGN TO THE GROUND SURFACE.

WHERE REMOVABLE SIGN POSTS ARE USED FOR OSOW VEHICLES, PLACE THE TOP OF THE 2 ¼" X 2 ¼" ANCHOR LEVEL WITH THE TOP OF THE 18" DIAMTER PVC BOX-OUT (WHICH IS AT GROUND LEVEL).

SIGNING LEGEND

	PROPOSED SIGN ON SINGLE/DOUBLE WOOD SUPPORT
	PROPOSED SIGN ON SINGLE/DOUBLE STEEL SUPPORT
02-XX	DENOTES SIGN NUMBER (CORRESPONDS TO MISCELLANEOUS QUANTITIES)

SHEET 3 OF 5

PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	PERMANENT SIGNING AND PAVEMENT MARKING	SHEET 45	E

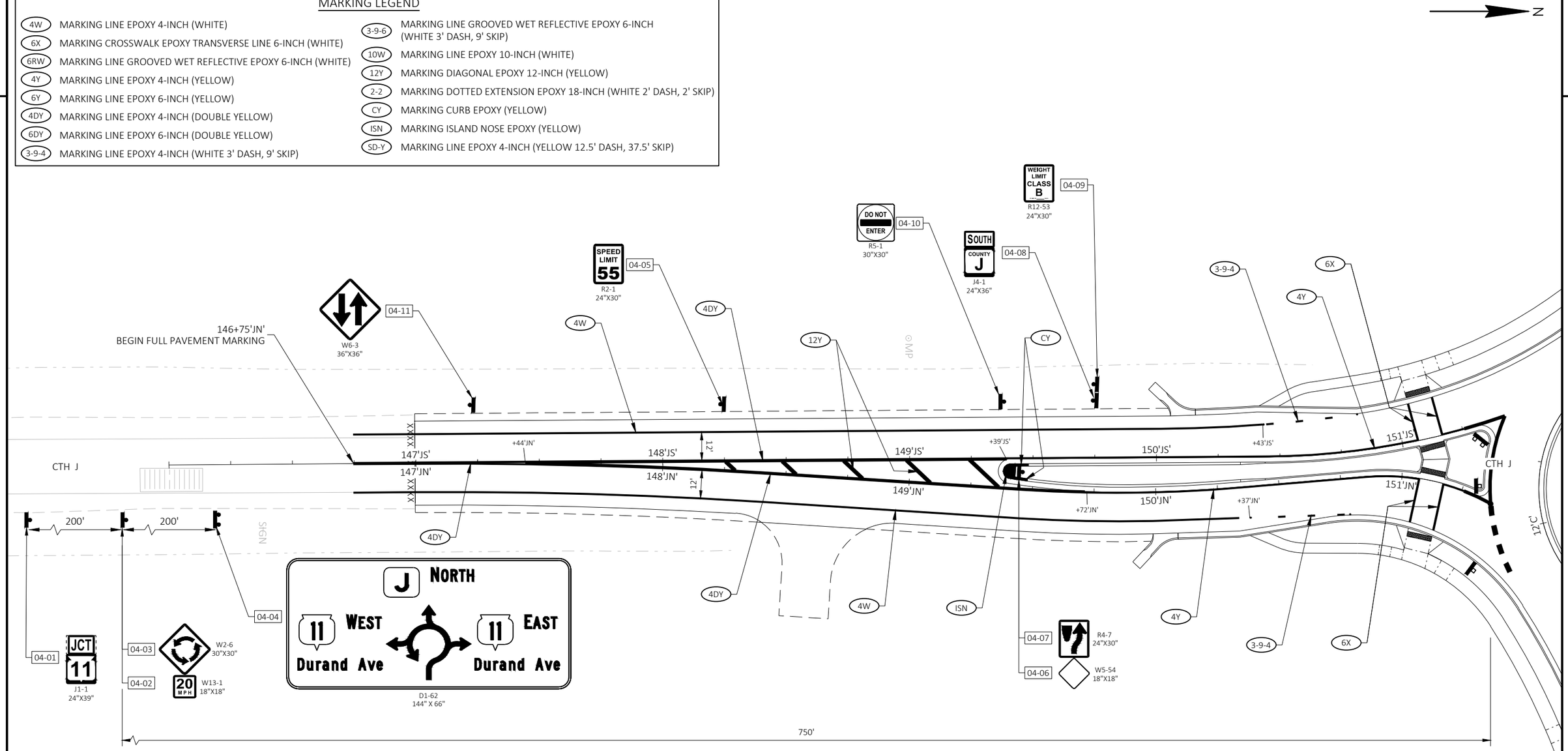
FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\023201PS-PM.DWG
LAYOUT NAME - 03

PLOT DATE : 10/27/2023 12:43 PM PLOT BY : KUSCHEL, LEVI PLOT NAME : PLOT SCALE : 1 IN:40 FT

2

MARKING LEGEND

- 4W — MARKING LINE EPOXY 4-INCH (WHITE)
- 6X — MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)
- 6RW — MARKING LINE GROOVED WET REFLECTIVE EPOXY 6-INCH (WHITE)
- 4Y — MARKING LINE EPOXY 4-INCH (YELLOW)
- 6Y — MARKING LINE EPOXY 6-INCH (YELLOW)
- 4DY — MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW)
- 6DY — MARKING LINE EPOXY 6-INCH (DOUBLE YELLOW)
- 3-9-4 — MARKING LINE EPOXY 4-INCH (WHITE 3' DASH, 9' SKIP)
- 3-9-6 — MARKING LINE GROOVED WET REFLECTIVE EPOXY 6-INCH (WHITE 3' DASH, 9' SKIP)
- 10W — MARKING LINE EPOXY 10-INCH (WHITE)
- 12Y — MARKING DIAGONAL EPOXY 12-INCH (YELLOW)
- 2-2 — MARKING DOTTED EXTENSION EPOXY 18-INCH (WHITE 2' DASH, 2' SKIP)
- CY — MARKING CURB EPOXY (YELLOW)
- ISN — MARKING ISLAND NOSE EPOXY (YELLOW)
- SD-Y — MARKING LINE EPOXY 4-INCH (YELLOW 12.5' DASH, 37.5' SKIP)



SIGNING LEGEND

- PROPOSED SIGN ON SINGLE/DOUBLE WOOD SUPPORT
- PROPOSED SIGN ON SINGLE/DOUBLE STEEL SUPPORT
- 02-XX — DENOTES SIGN NUMBER (CORRESPONDS TO MISCELLANEOUS QUANTITIES)

NOTES

YIELD SIGNS SHALL BE MOUNTED AT THE SAME HEIGHT, 7'3" FROM THE BOTTOM OF THE SIGN TO THE GROUND SURFACE.

WHERE REMOVABLE SIGN POSTS ARE USED FOR OSOW VEHICLES, PLACE THE TOP OF THE 2 ¼" X 2 ¼" ANCHOR LEVEL WITH THE TOP OF THE 18" DIAMTER PVC BOX-OUT (WHICH IS AT GROUND LEVEL).

SHEET 4 OF 5

PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	PERMANENT SIGNING AND PAVEMENT MARKING	SHEET 46	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\023201PS-PM.DWG
LAYOUT NAME - 04

PLOT DATE : 10/27/2023 12:43 PM PLOT BY : KUSCHEL, LEVI PLOT NAME : PLOT SCALE : 1 IN:40 FT

WISDOT/CADDS SHEET 42

2

N

750'

J SOUTH

11 EAST Durand Ave

11 WEST Durand Ave

D1-62
144" X 66"

W2-6
30"X30"

20 MPH W13-1
18"X18"

JCT 11 J1-1
24"X39"

05-06 05-07 05-08 05-09

200' 200'

157+11'JN'
END FULL PAVEMENT MARKING

CTH J

153'JS' 154'JS' +94'JS' 155'JS' 156'JS' 156'JS' 157'JN' 158'JN'

153'JN' 154'JN' +80'JN' 155'JN' 156'JN' +54'JN'

12' 12'

6X 3-9-4 SD-Y 4Y ISN CY ISN 4DY 12Y 4W 4DY 4W 4DY

05-01 R4-7 24"X30"

NORTH COUNTY J J4-1 24"X36" 05-05

R4-7 24"X30" 05-02

05-04 R4-7 24"X30"

05-03 W5-54 18"X18"

05-10 DO NOT ENTER R5-1 30"X30"

05-11 SPEED LIMIT 55 R2-1 24"X30"

05-12 W6-3 36"X36"

MARKING LEGEND

4W	MARKING LINE EPOXY 4-INCH (WHITE)
6X	MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)
6RW	MARKING LINE GROOVED WET REFLECTIVE EPOXY 6-INCH (WHITE)
4Y	MARKING LINE EPOXY 4-INCH (YELLOW)
6Y	MARKING LINE EPOXY 6-INCH (YELLOW)
4DY	MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW)
6DY	MARKING LINE EPOXY 6-INCH (DOUBLE YELLOW)
3-9-4	MARKING LINE EPOXY 4-INCH (WHITE 3' DASH, 9' SKIP)
3-9-6	MARKING LINE GROOVED WET REFLECTIVE EPOXY 6-INCH (WHITE 3' DASH, 9' SKIP)
10W	MARKING LINE EPOXY 10-INCH (WHITE)
12Y	MARKING DIAGONAL EPOXY 12-INCH (YELLOW)
2-2	MARKING DOTTED EXTENSION EPOXY 18-INCH (WHITE 2' DASH, 2' SKIP)
CY	MARKING CURB EPOXY (YELLOW)
ISN	MARKING ISLAND NOSE EPOXY (YELLOW)
SD-Y	MARKING LINE EPOXY 4-INCH (YELLOW 12.5' DASH, 37.5' SKIP)

NOTES

YIELD SIGNS SHALL BE MOUNTED AT THE SAME HEIGHT, 7'3" FROM THE BOTTOM OF THE SIGN TO THE GROUND SURFACE.

WHERE REMOVABLE SIGN POSTS ARE USED FOR OSOW VEHICLES, PLACE THE TOP OF THE 2 ¼" X 2 ¼" ANCHOR LEVEL WITH THE TOP OF THE 18" DIAMTER PVC BOX-OUT (WHICH IS AT GROUND LEVEL).

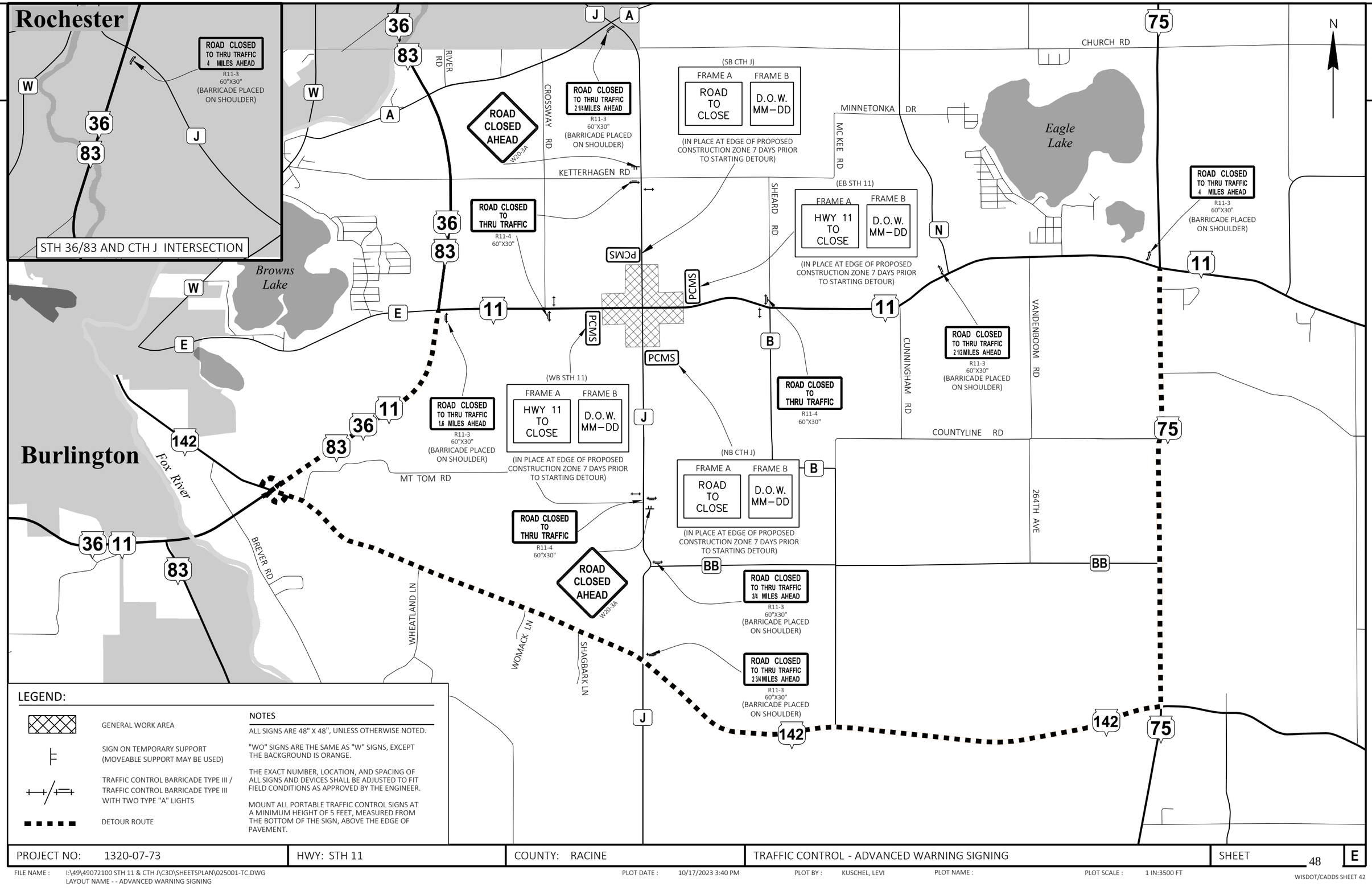
SIGNING LEGEND

- PROPOSED SIGN ON SINGLE/DOUBLE WOOD SUPPORT
- PROPOSED SIGN ON SINGLE/DOUBLE STEEL SUPPORT
- 02-XX DENOTES SIGN NUMBER (CORRESPONDS TO MISCELLANEOUS QUANTITIES)

SHEET 5 OF 5

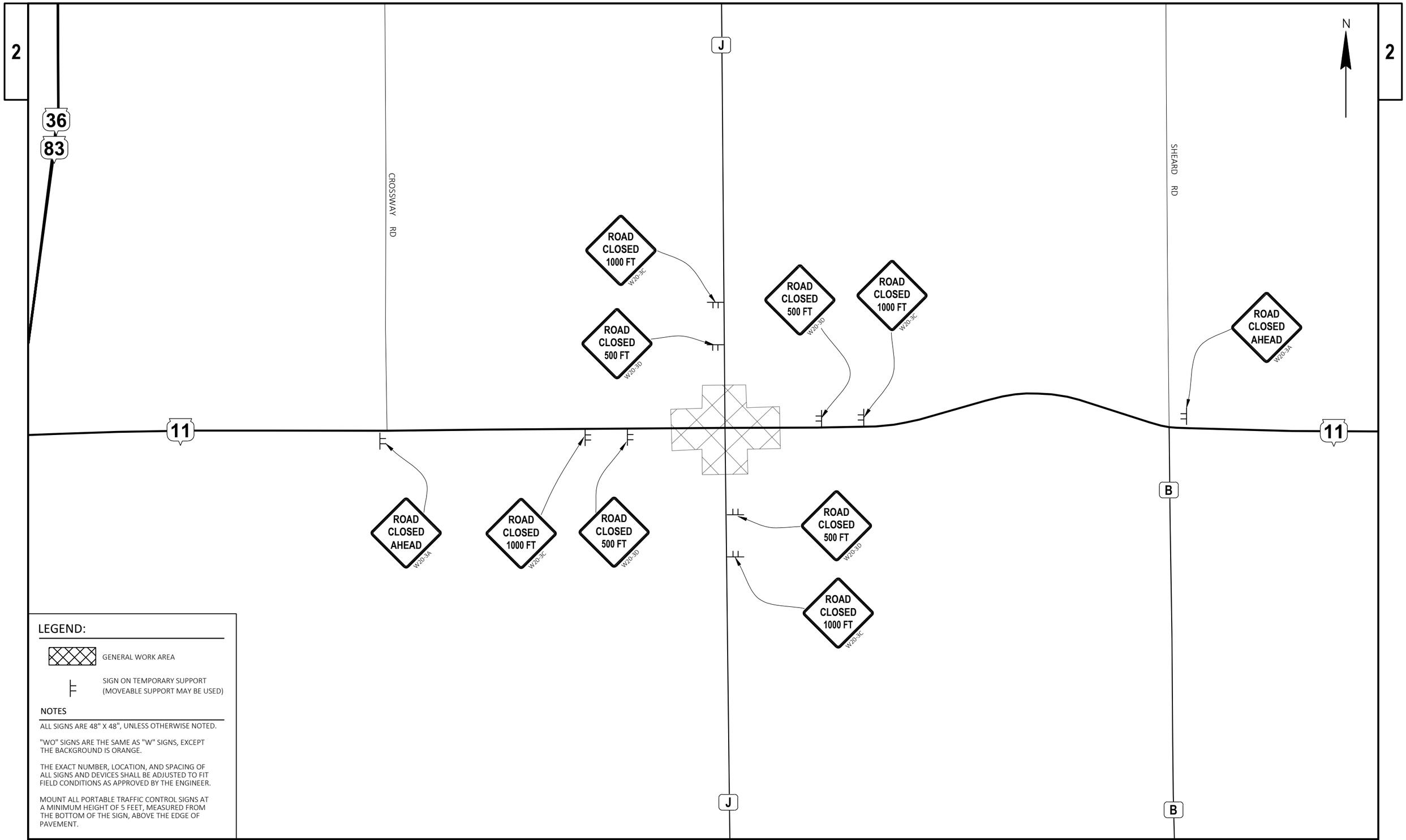
PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	PERMANENT SIGNING AND PAVEMENT MARKING	SHEET 47	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\023201PS-PM.DWG
LAYOUT NAME - 05
PLOT DATE : 10/27/2023 12:43 PM PLOT BY : KUSCHEL, LEVI PLOT NAME : PLOT SCALE : 1 IN:40 FT
WISDOT/CADDS SHEET 42



PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	TRAFFIC CONTROL - ADVANCED WARNING SIGNING	SHEET 48 E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\025001-TC.DWG
LAYOUT NAME - - ADVANCED WARNING SIGNING
PLOT DATE : 10/17/2023 3:40 PM PLOT BY : KUSCHEL, LEVI PLOT NAME : PLOT SCALE : 1 IN:3500 FT



LEGEND:

GENERAL WORK AREA

SIGN ON TEMPORARY SUPPORT
(MOVEABLE SUPPORT MAY BE USED)

NOTES

ALL SIGNS ARE 48" X 48", UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS, EXCEPT THE BACKGROUND IS ORANGE.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

MOUNT ALL PORTABLE TRAFFIC CONTROL SIGNS AT A MINIMUM HEIGHT OF 5 FEET, MEASURED FROM THE BOTTOM OF THE SIGN, ABOVE THE EDGE OF PAVEMENT.

PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	TRAFFIC CONTROL - ADVANCED WARNING SIGNING	SHEET 49	E

2

N

R11-4
60" X 30"
ROAD CLOSED TO THRU TRAFFIC
TYPE III BARRICADE

CTH J

MAINTAIN ACCESS TO DRIVEWAYS WITH DRUMS
THROUGH NORTH LEG OF CONSTRUCTION ZONE

RESTAURANT

R11-2
48" X 30"
ROAD CLOSED
TYPE III BARRICADE

R11-2
48" X 30"
ROAD CLOSED
TYPE III BARRICADE

STH 11

STH 11

CONSTRUCTION ZONE

R11-2
48" X 30"
ROAD CLOSED
TYPE III BARRICADE

R11-2
48" X 30"
ROAD CLOSED
TYPE III BARRICADE

CTH J

PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	TRAFFIC CONTROL - ADVANCED WARNING SIGNING	SHEET 50	E

FILE NAME : \\AYRES_ACTIVE\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\025003-TC.DWG
LAYOUT NAME - - ADVANCED WARNING SIGNING

PLOT DATE : 10/27/2023 1:08 PM PLOT BY : KUSCHEL, LEVI PLOT NAME : PLOT SCALE : 1 IN:120 FT

LEGEND:

- DETOUR ROUTE
- WORK AREA
- TRAFFIC CONTROL SIGN ON TEMPORARY SUPPORT
- EXISTING SIGN ON PERMANENT SUPPORT
- EXISTING SIGN, COVER EXISTING SIGN, PROPOSED DETOUR SIGN

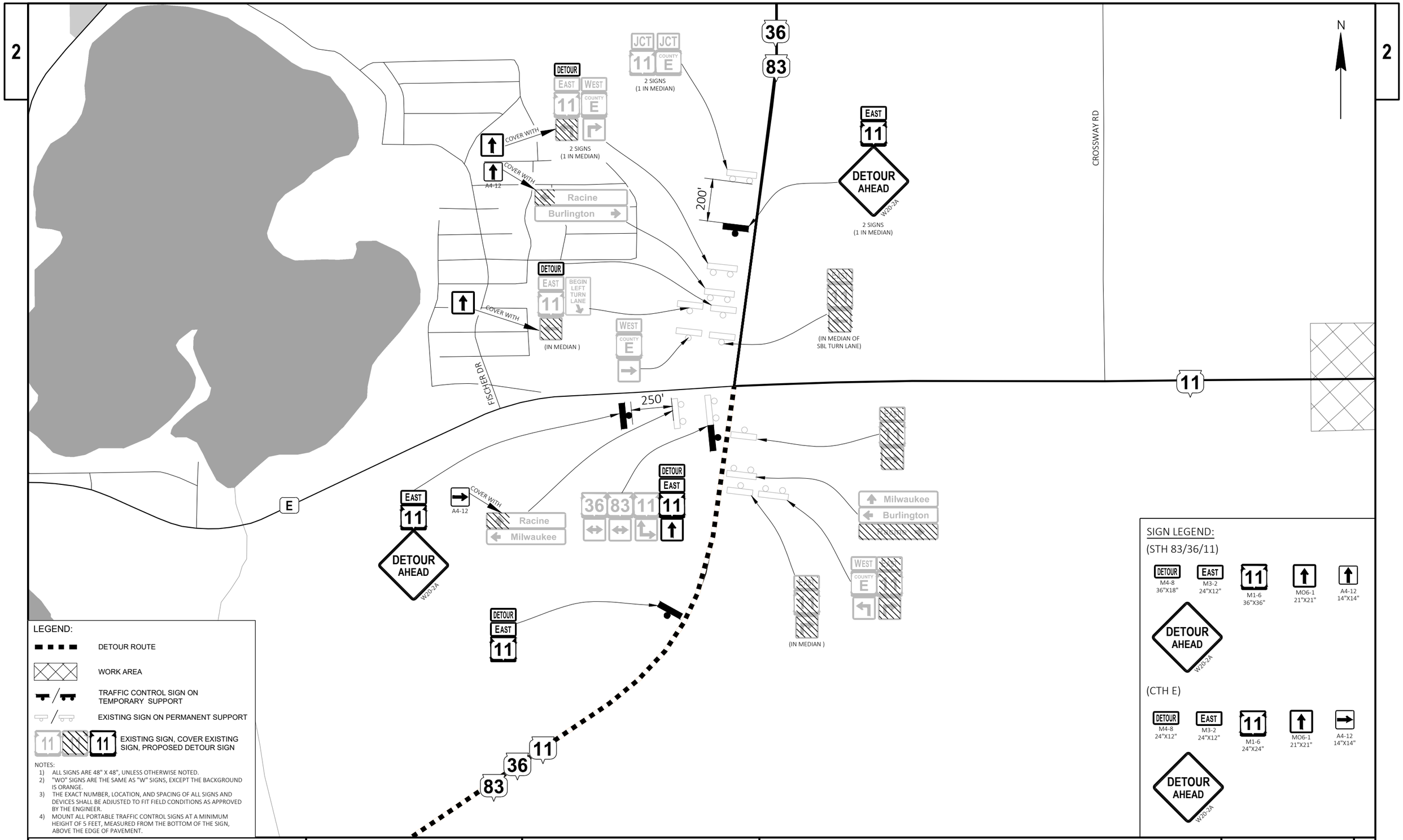
NOTES:

1) ALL SIGNS ARE 48" X 48", UNLESS OTHERWISE NOTED.
2) "WO" SIGNS ARE THE SAME AS "W" SIGNS, EXCEPT THE BACKGROUND IS ORANGE.
3) THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.
4) MOUNT ALL PORTABLE TRAFFIC CONTROL SIGNS AT A MINIMUM HEIGHT OF 5 FEET, MEASURED FROM THE BOTTOM OF THE SIGN, ABOVE THE EDGE OF PAVEMENT.

Rochester
Burlington
Browns Lake
Eagle Lake
Fox River
CHURCH RD
MINNETONKA DR
KETTERHAGEN RD
MT TOM RD
COUNTYLINE RD
RIVER RD
S WESTWOOD AVE
CROSSWAY RD
MC KEE RD
SHEARD RD
CUNNINGHAM RD
VANDENBOOM RD
264TH AVE
BREVER RD
WHEATLAND LN
WOMACK LN
SHAGBARK LN
SHEET 01
SHEET 02
SHEET 03
SHEET 04
SHEET 05
N

PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	STH 11 DETOUR	SHEET 51 E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\027001-DT.DWG
LAYOUT NAME - - STH 11
PLOT DATE : 10/5/2023 3:39 PM
PLOT BY : KUSCHEL, LEVI
PLOT NAME :
PLOT SCALE : 1 IN:3500 FT



PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	STH 11 DETOUR - SHEET 01	SHEET 52	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\027001-DT.DWG
LAYOUT NAME - - SHEET 01

PLOT DATE : 10/5/2023 3:39 PM PLOT BY : KUSCHEL, LEVI PLOT NAME : PLOT SCALE : 1 IN:1000 FT

2

LEGEND:

- DETOUR ROUTE
- WORK AREA
- TRAFFIC CONTROL SIGN ON TEMPORARY SUPPORT
- EXISTING SIGN ON PERMANENT SUPPORT
- EXISTING SIGN, COVER EXISTING SIGN, PROPOSED DETOUR SIGN

NOTES:
1) ALL SIGNS ARE 48" X 48", UNLESS OTHERWISE NOTED.
2) "WO" SIGNS ARE THE SAME AS "W" SIGNS, EXCEPT THE BACKGROUND IS ORANGE.
3) THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.
4) MOUNT ALL PORTABLE TRAFFIC CONTROL SIGNS AT A MINIMUM HEIGHT OF 5 FEET, MEASURED FROM THE BOTTOM OF THE SIGN, ABOVE THE EDGE OF PAVEMENT.

N

BREVER RD

MT TOM RD

EAST 11 — DETOUR AHEAD W20-2A

JCT 11 36 83

WEST SOUTH SOUTH 11 36 83

DETOUR EAST 11 — EAST 142

WEST SOUTH SOUTH 11 36 83

142

DETOUR EAST 142 11 — IN RAMP MEDIAN

JCT 142 — 2 SIGNS (1 IN MEDIAN)

DETOUR EAST 142 11 — 2 SIGNS (1 IN MEDIAN)

NORTH NORTH 36 83

WEST SOUTH SOUTH 11 36 83

NORTH NORTH 36 83

END DETOUR WEST 11

DETOUR EAST NORTH NORTH 11 36 83 — COVER WITH

36 11 — 3780'

DETOUR EAST 142 11 — 2 SIGNS (1 IN MEDIAN)

DETOUR EAST 142 11 — IN RAMP MEDIAN

DETOUR EAST NORTH NORTH 11 36 83 — COVER WITH

DETOUR EAST 142 11

DETOUR EAST 142 11

DETOUR NEXT 11 MILES

NORTH NORTH 36 83

142

EAST 11 — DETOUR 1 MILE W20-2F — 2 SIGNS (1 IN MEDIAN)

JCT EAST 142 11 — DETOUR 1500 FT W20-2B — 2 SIGNS (1 IN MEDIAN)

SIGN LEGEND:

(STH 83/36/11)

DETOUR M4-8 36"X18" — EAST M3-2 24"X12" — 11 M1-6 36"X36" — MO5-1R 21"X21" — MO6-1 30"X30"

DETOUR 1500 FT W20-2B — DETOUR 1 MILE W20-2F

(STH 142)

DETOUR M4-8 24"X12" — EAST M3-2 24"X12" — WEST M3-4 24"X12" — 11 M1-6 24"X24" — END DETOUR M4-8A 24"X18"

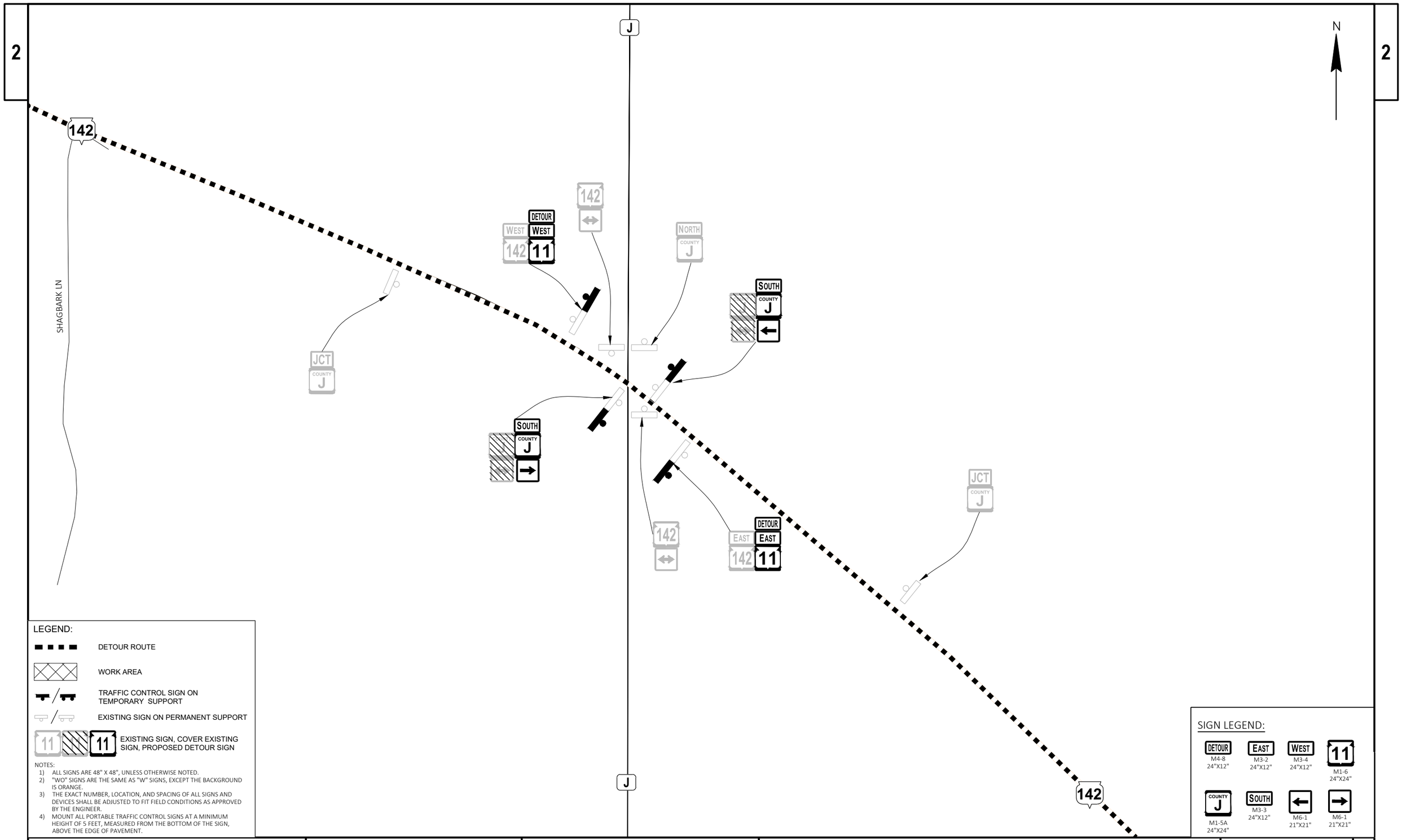
DETOUR AHEAD W20-2A — DETOUR NEXT 11 MILES G20-51 60"X24" — MO6-1 21"X21" — A4-12 14"X14"

PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	STH 11 DETOUR - SHEET 02	SHEET 53	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\027001-DT.DWG
LAYOUT NAME - - SHEET 02

PLOT DATE : 10/5/2023 3:39 PM PLOT BY : KUSCHEL, LEVI PLOT NAME : PLOT SCALE : 1 IN:500 FT

WISDOT/CADDS SHEET 42



LEGEND:

- DETOUR ROUTE
- WORK AREA
- TRAFFIC CONTROL SIGN ON TEMPORARY SUPPORT
- EXISTING SIGN ON PERMANENT SUPPORT
- 11 → 11 EXISTING SIGN, COVER EXISTING SIGN, PROPOSED DETOUR SIGN

NOTES:

1) ALL SIGNS ARE 48" X 48", UNLESS OTHERWISE NOTED.
2) "WO" SIGNS ARE THE SAME AS "W" SIGNS, EXCEPT THE BACKGROUND IS ORANGE.
3) THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.
4) MOUNT ALL PORTABLE TRAFFIC CONTROL SIGNS AT A MINIMUM HEIGHT OF 5 FEET, MEASURED FROM THE BOTTOM OF THE SIGN, ABOVE THE EDGE OF PAVEMENT.

SIGN LEGEND:

DETOUR	EAST	WEST	11
M4-8 24"X12"	M3-2 24"X12"	M3-4 24"X12"	M1-6 24"X24"

COUNTY J	SOUTH	←	→
M1-5A 24"X24"	M3-3 24"X12"	M6-1 21"X21"	M6-1 21"X21"

PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	STH 11 DETOUR - SHEET 03	SHEET 54 E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\027001-DT.DWG
LAYOUT NAME - - SHEET 03

PLOT DATE : 10/5/2023 3:39 PM PLOT BY : KUSCHEL, LEVI PLOT NAME : PLOT SCALE : 1 IN:400 FT

2

N

75

JCT 142

EAST WEST DETOUR WEST 142 142 11

NORTH DETOUR EAST 75 11

SOUTH NORTH DETOUR WEST 75 75 11

SOUTH WEST NORTH DETOUR WEST 75 142 75 11

EAST WEST DETOUR WEST 142 142 11

WEST DETOUR WEST 142 11

JCT 75

WEST 11 DETOUR AHEAD W20-2A

500'

142

240TH AVE

DETOUR WEST WEST EAST 11 142 142

DETOUR EAST NORTH SOUTH 11 75 75

JCT 75

DETOUR EAST NORTH EAST SOUTH 11 75 142 75

DETOUR WEST WEST EAST 11 142 142

1000'

WEST 11 DETOUR AHEAD W20-2A

75

LEGEND:

- DETOUR ROUTE
- WORK AREA
- TRAFFIC CONTROL SIGN ON TEMPORARY SUPPORT
- EXISTING SIGN ON PERMANENT SUPPORT
- 11 / 11 — EXISTING SIGN, COVER EXISTING SIGN, PROPOSED DETOUR SIGN

NOTES:
1) ALL SIGNS ARE 48" X 48", UNLESS OTHERWISE NOTED.
2) "WO" SIGNS ARE THE SAME AS "W" SIGNS, EXCEPT THE BACKGROUND IS ORANGE.
3) THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.
4) MOUNT ALL PORTABLE TRAFFIC CONTROL SIGNS AT A MINIMUM HEIGHT OF 5 FEET, MEASURED FROM THE BOTTOM OF THE SIGN, ABOVE THE EDGE OF PAVEMENT.

SIGN LEGEND:

DETOUR M4-8 24"X12" | EAST M3-2 24"X12" | WEST M3-4 24"X12" | 11 M1-6 24"X24"

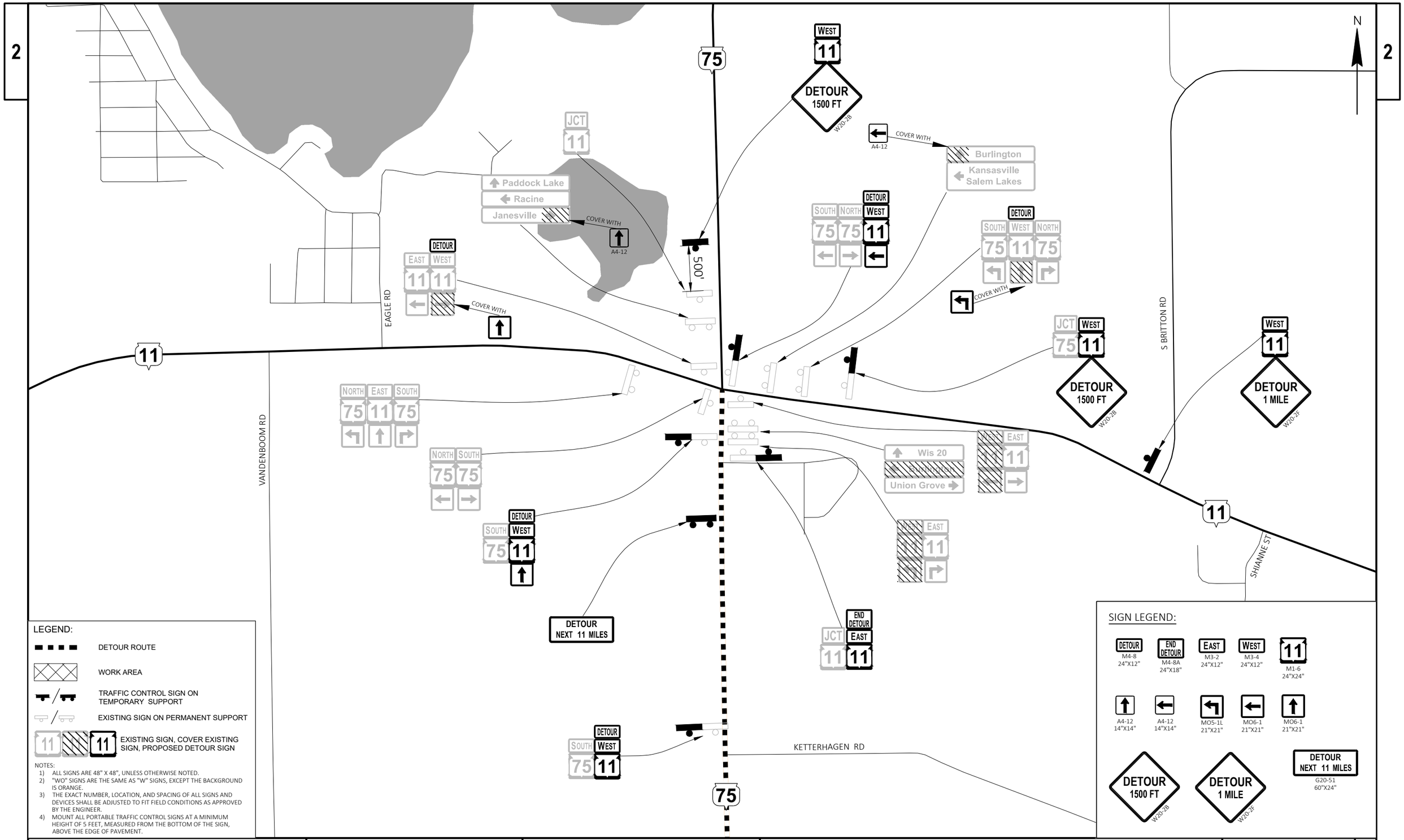
MO5-1L 21"X21" | MO5-1R 21"X21" | MO6-1 21"X21" | MO6-1 21"X21" | MO6-1 21"X21"

DETOUR AHEAD W20-2A

PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	STH 11 DETOUR - SHEET 04	SHEET 55	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\027001-DT.DWG
LAYOUT NAME - - SHEET 04

PLOT DATE : 10/5/2023 3:39 PM PLOT BY : KUSCHEL, LEVI PLOT NAME : PLOT SCALE : 1 IN:500 FT



PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	STH 11 DETOUR - SHEET 05	SHEET 56	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\027001-DT.DWG
LAYOUT NAME - - SHEET 05
PLOT DATE : 10/5/2023 3:39 PM
PLOT BY : KUSCHEL, LEVI
PLOT NAME :
PLOT SCALE : 1 IN:1000 FT

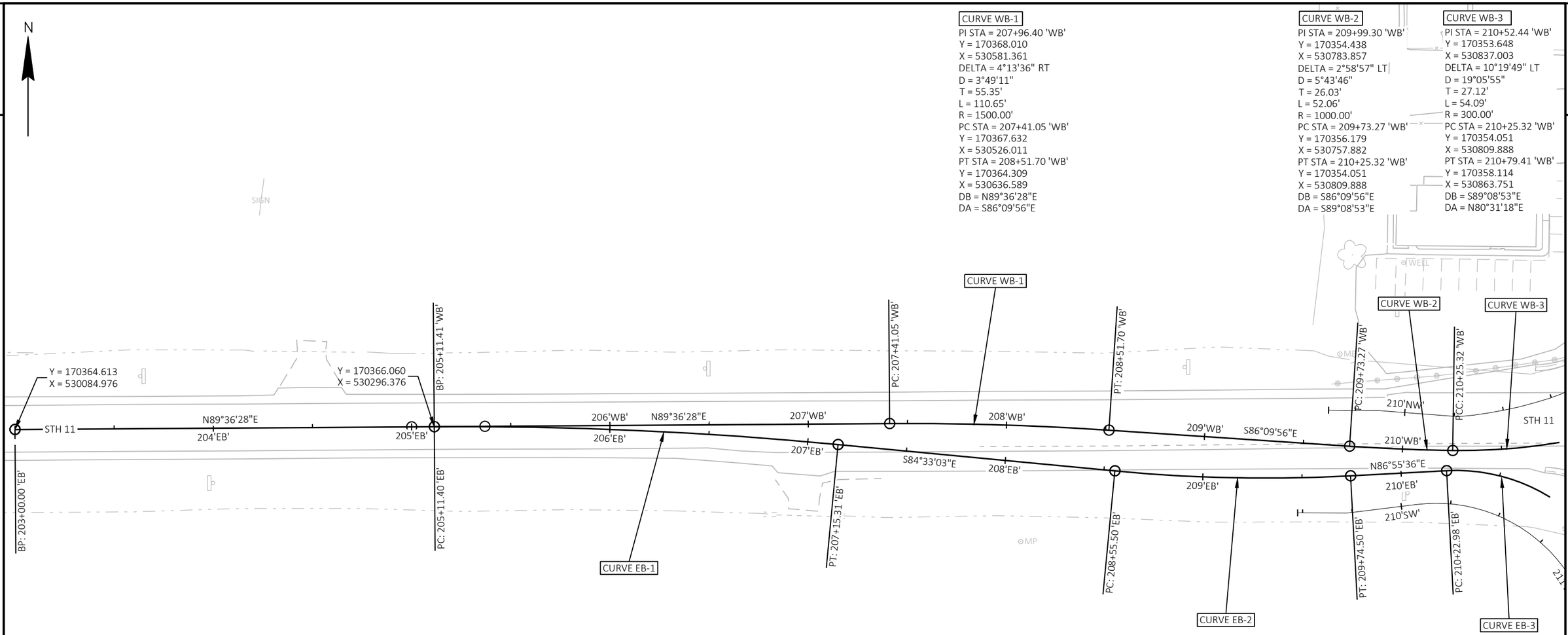
2

N

CURVE WB-1
PI STA = 207+96.40 'WB'
Y = 170368.010
X = 530581.361
DELTA = 4°13'36" RT
D = 3°49'11"
T = 55.35'
L = 110.65'
R = 1500.00'
PC STA = 207+41.05 'WB'
Y = 170367.632
X = 530526.011
PT STA = 208+51.70 'WB'
Y = 170364.309
X = 530636.589
DB = N89°36'28"E
DA = S86°09'56"E

CURVE WB-2
PI STA = 209+99.30 'WB'
Y = 170354.438
X = 530783.857
DELTA = 2°58'57" LT
D = 5°43'46"
T = 26.03'
L = 52.06'
R = 1000.00'
PC STA = 209+73.27 'WB'
Y = 170356.179
X = 530757.882
PT STA = 210+25.32 'WB'
Y = 170354.051
X = 530809.888
DB = S86°09'56"E
DA = S89°08'53"E

CURVE WB-3
PI STA = 210+52.44 'WB'
Y = 170353.648
X = 530837.003
DELTA = 10°19'49" LT
D = 19°05'55"
T = 27.12'
L = 54.09'
R = 300.00'
PC STA = 210+25.32 'WB'
Y = 170354.051
X = 530809.888
PT STA = 210+79.41 'WB'
Y = 170358.114
X = 530863.751
DB = S89°08'53"E
DA = N80°31'18"E



CURVE EB-1
PI STA = 206+13.44 'EB'
Y = 170366.758
X = 530398.412
DELTA = 5°50'29" RT
D = 2°51'53"
T = 102.04'
L = 203.91'
R = 2000.00'
PC STA = 205+11.40 'EB'
Y = 170366.060
X = 530296.373
PT STA = 207+15.31 'EB'
Y = 170357.068
X = 530499.993
DB = N89°36'28"E
DA = S84°33'03"E

CURVE EB-2
PI STA = 209+15.11 'EB'
Y = 170338.094
X = 530698.888
DELTA = 8°31'22" LT
D = 7°09'43"
T = 59.61'
L = 119.00'
R = 800.00'
PC STA = 208+55.50 'EB'
Y = 170343.755
X = 530639.547
PT STA = 209+74.50 'EB'
Y = 170341.290
X = 530758.412
DB = S84°33'03"E
DA = N86°55'36"E

CURVE EB-3
PI STA = 210+51.20 'EB'
Y = 170345.403
X = 530835.006
DELTA = 34°49'29" RT
D = 63°39'43"
T = 28.23'
L = 54.70'
R = 90.00'
PC STA = 210+22.98 'EB'
Y = 170343.889
X = 530806.821
PT STA = 210+77.68 'EB'
Y = 170330.550
X = 530859.008
DB = N86°55'36"E
DA = S58°14'55"E

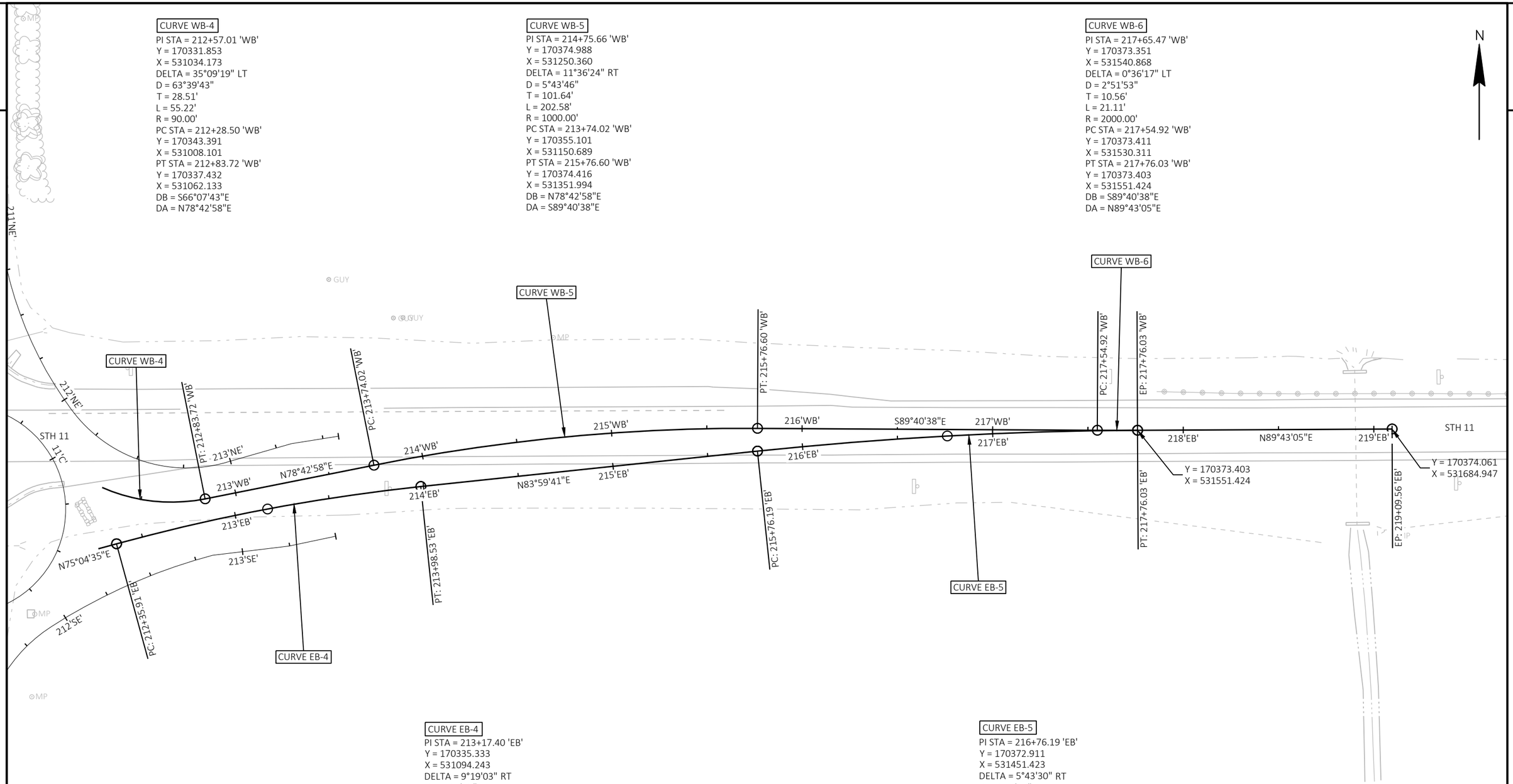
PROJECT NO: 1320-07-03	HWY: STH 11	COUNTY: RACINE	ALIGNMENT DIAGRAM	SHEET 57	E

2

CURVE WB-4
PI STA = 212+57.01 'WB'
Y = 170331.853
X = 531034.173
DELTA = 35°09'19" LT
D = 63°39'43"
T = 28.51'
L = 55.22'
R = 90.00'
PC STA = 212+28.50 'WB'
Y = 170343.391
X = 531008.101
PT STA = 212+83.72 'WB'
Y = 170337.432
X = 531062.133
DB = S66°07'43"E
DA = N78°42'58"E

CURVE WB-5
PI STA = 214+75.66 'WB'
Y = 170374.988
X = 531250.360
DELTA = 11°36'24" RT
D = 5°43'46"
T = 101.64'
L = 202.58'
R = 1000.00'
PC STA = 213+74.02 'WB'
Y = 170355.101
X = 531150.689
PT STA = 215+76.60 'WB'
Y = 170374.416
X = 531351.994
DB = N78°42'58"E
DA = S89°40'38"E

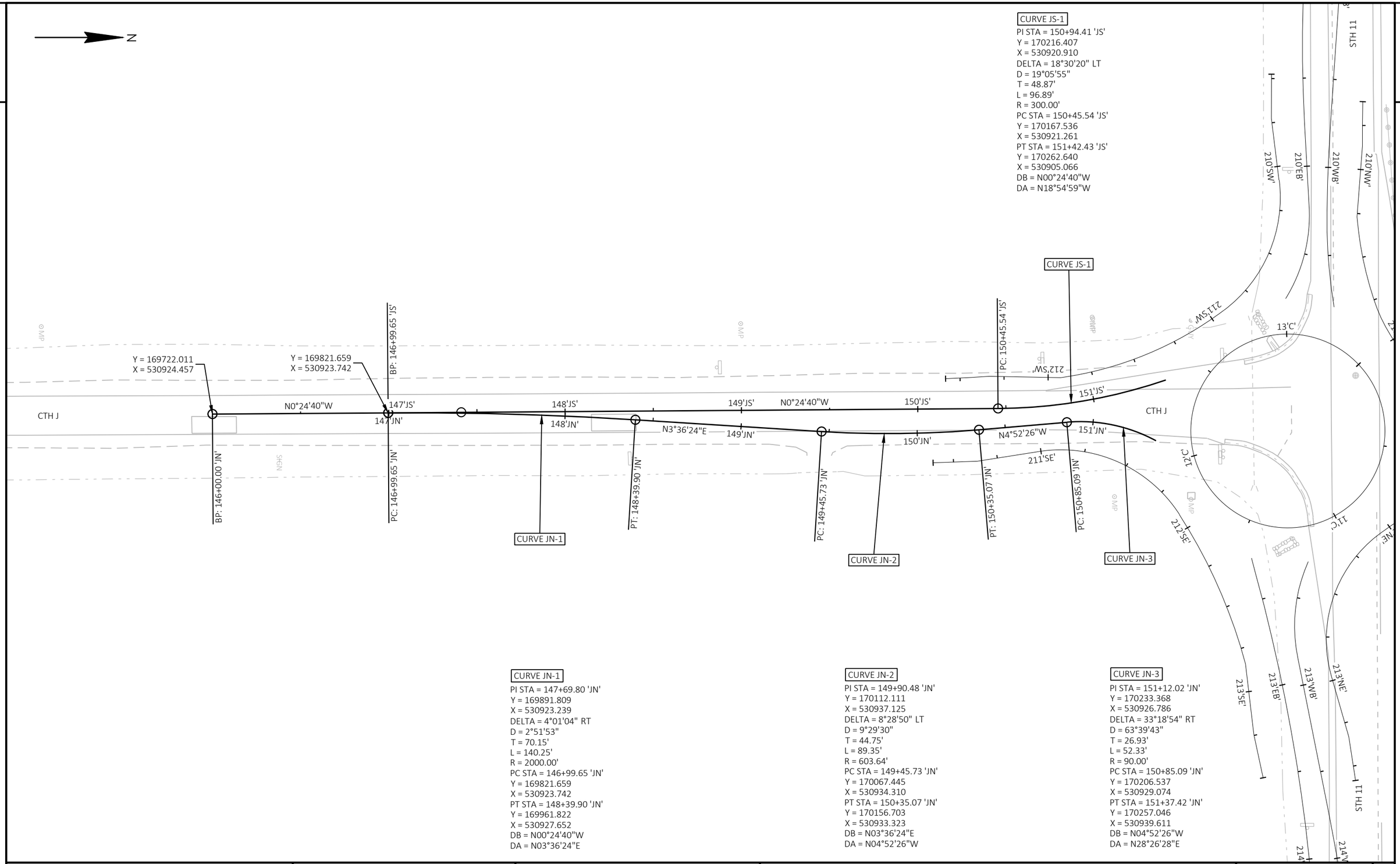
CURVE WB-6
PI STA = 217+65.47 'WB'
Y = 170373.351
X = 531540.868
DELTA = 0°36'17" LT
D = 2°51'53"
T = 10.56'
L = 21.11'
R = 2000.00'
PC STA = 217+54.92 'WB'
Y = 170373.411
X = 531530.311
PT STA = 217+76.03 'WB'
Y = 170373.403
X = 531551.424
DB = S89°40'38"E
DA = N89°43'05"E



CURVE EB-4
PI STA = 213+17.40 'EB'
Y = 170335.333
X = 531094.243
DELTA = 9°19'03" RT
D = 5°43'46"
T = 81.49'
L = 162.62'
R = 1000.00'
PC STA = 212+35.91 'EB'
Y = 170313.799
X = 531015.650
PT STA = 213+98.53 'EB'
Y = 170343.859
X = 531175.285
DB = N74°40'38"E
DA = N83°59'41"E

CURVE EB-5
PI STA = 216+76.19 'EB'
Y = 170372.911
X = 531451.423
DELTA = 5°43'30" RT
D = 2°51'53"
T = 100.00'
L = 199.84'
R = 2000.00'
PC STA = 215+76.19 'EB'
Y = 170362.446
X = 531351.969
PT STA = 217+76.03 'EB'
Y = 170373.403
X = 531551.424
DB = N83°59'35"E
DA = N89°43'05"E

PROJECT NO: 1320-07-03	HWY: STH 11	COUNTY: RACINE	ALIGNMENT DIAGRAM	SHEET 58	E



PROJECT NO: 1320-07-03	HWY: STH 11	COUNTY: RACINE	ALIGNMENT DIAGRAM	SHEET 59	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\027202-AD.DWG
LAYOUT NAME - CTH J-1

PLOT DATE : 5/30/2023 1:53 PM PLOT BY : MUENCH, DOUGLAS PLOT NAME : PLOT SCALE : 1 IN:50 FT

2

N

CURVE JS-2
PI STA = 153+03.41 'JS'
Y = 170416.589
X = 530935.174
DELTA = 44°50'39" LT
D = 71°37'11"
T = 33.01'
L = 62.61'
R = 80.00'
PC STA = 152+70.40 'JS'
Y = 170390.305
X = 530915.205
PT STA = 153+33.01 'JS'
Y = 170449.308
X = 530930.797
DB = N37°13'28"E
DA = N07°37'11"W

CURVE JS-3
PI STA = 154+57.06 'JS'
Y = 170572.255
X = 530914.349
DELTA = 7°34'19" RT
D = 7°09'43"
T = 52.94'
L = 105.72'
R = 800.00'
PC STA = 154+04.12 'JS'
Y = 170519.783
X = 530921.368
PT STA = 155+09.84 'JS'
Y = 170625.195
X = 530914.305
DB = N07°37'11"W
DA = N00°02'52"W

CURVE JS-4
PI STA = 156+73.72 'JS'
Y = 170789.076
X = 530914.168
DELTA = 0°43'25" LT
D = 2°51'53"
T = 12.63'
L = 25.26'
R = 2000.00'
PC STA = 156+61.09 'JS'
Y = 170776.447
X = 530914.178
PT STA = 156+86.35 'JS'
Y = 170801.704
X = 530913.998
DB = N00°02'52"W
DA = N00°46'17"W

STH 11
209'NW'
210'NW'
211'NW'
212'NW'
CTH J
153'JS'
154'JS'
155'JS'
156'JS'
157'JN'
158'JN'
159'JN'
N7°37'11"W
N0°02'52"W
N0°46'17"W
154'JN'
153'JN'
155'JN'
156'JN'
N5°54'12"W
PT: 153+33.01 'JS'
PC: 154+04.12 'JS'
PT: 155+09.84 'JS'
PC: 156+61.09 'JS'
EP: 156+86.35 'JS'
Y = 171021.912
X = 530911.033
EP: 159+06.60 'JN'
PT: 156+86.35 'JN'
PC: 155+07.22 'JN'
PT: 153+60.07 'JN'
212'NE'
213'NE'
CURVE JS-2
CURVE JS-3
CURVE JS-4
CURVE JN-4
CURVE JN-5
WELL
SEPTIC
A/C
SIGN
MP

CURVE JN-4
PI STA = 153+22.61 'JN'
Y = 170438.907
X = 530943.450
DELTA = 14°23'09" RT
D = 19°05'55"
T = 37.86'
L = 75.32'
R = 300.00'
PC STA = 152+84.75 'JN'
Y = 170403.395
X = 530956.579
PT STA = 153+60.07 'JN'
Y = 170476.568
X = 530939.556
DB = N20°17'21"W
DA = N05°54'12"W

CURVE JN-5
PI STA = 155+96.84 'JN'
Y = 170712.083
X = 530915.205
DELTA = 5°07'55" RT
D = 2°51'53"
T = 89.63'
L = 179.14'
R = 2000.00'
PC STA = 155+07.22 'JN'
Y = 170622.930
X = 530924.423
PT STA = 156+86.35 'JN'
Y = 170801.704
X = 530913.998
DB = N05°54'12"W
DA = N00°46'17"W

PROJECT NO: 1320-07-03	HWY: STH 11	COUNTY: RACINE	ALIGNMENT DIAGRAM	SHEET 60	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\027202-AD.DWG
LAYOUT NAME - CTH J-2
PLOT DATE : 5/30/2023 1:53 PM
PLOT BY : MUENCH, DOUGLAS
PLOT NAME :
PLOT SCALE : 1 IN:50 FT
WISDOT/CADDS SHEET 42

CURVE C-1
PI STA = 440360+05.16 'C'
Y = 30306568.197
X = 32638393.208
DELTA = 179°59'59" RT
D = 104°10'27"
T = 44034995.12'
L = 172.79'
R = 55.00'
PC STA = 10+00.00 'C'
Y = 170371.938
X = 530896.356
PT STA = 11+72.79 'C'
Y = 170291.734
X = 530971.637
DB = N46°48'50"E
DA = S46°48'50"W

CURVE C-2
PI STA = 469199+84.89 'C'
Y = -31939495.474
X = -33679209.510
DELTA = 180°00'00" RT
D = 104°10'27"
T = 46918804.24'
L = 172.79'
R = 55.00'
PC STA = 11+72.79 'C'
Y = 170291.734
X = 530971.637
PT STA = 13+45.57 'C'
Y = 170371.938
X = 530896.356
DB = S46°48'50"W
DA = N46°48'50"E

CURVE CC-1
PI STA = 20+04.45 'CC'
Y = 170360.999
X = 530912.399
DELTA = 20°10'49" RT
D = 229°11'06"
T = 4.45'
L = 8.81'
R = 25.00'
PC STA = 20+00.00 'CC'
Y = 170359.164
X = 530908.346
PT STA = 20+08.81 'CC'
Y = 170361.323
X = 530916.836
DB = N65°38'22"E
DA = N85°49'11"E

CURVE CC-2
PI STA = 20+26.48 'CC'
Y = 170362.612
X = 530934.464
DELTA = 10°06'03" RT
D = 28°38'52"
T = 17.68'
L = 35.26'
R = 200.00'
PC STA = 20+08.81 'CC'
Y = 170361.323
X = 530916.836
PT STA = 20+44.06 'CC'
Y = 170360.789
X = 530952.044
DB = N85°49'09"E
DA = S84°04'48"E

CURVE CC-3
PI STA = 20+61.34 'CC'
Y = 170359.007
X = 530969.225
DELTA = 69°16'59" RT
D = 229°10'59"
T = 17.27'
L = 30.23'
R = 25.00'
PC STA = 20+44.06 'CC'
Y = 170360.789
X = 530952.044
PT STA = 20+74.29 'CC'
Y = 170342.307
X = 530973.637
DB = S84°04'48"E
DA = S14°47'48"E

CURVE CC-4
PI STA = 20+85.79 'CC'
Y = 170331.190
X = 530976.574
DELTA = 31°19'58" RT
D = 139°44'45"
T = 11.50'
L = 22.42'
R = 41.00'
PC STA = 20+74.29 'CC'
Y = 170342.307
X = 530973.637
PT STA = 20+96.72 'CC'
Y = 170320.167
X = 530973.301
DB = S14°47'48"E
DA = S16°32'10"W

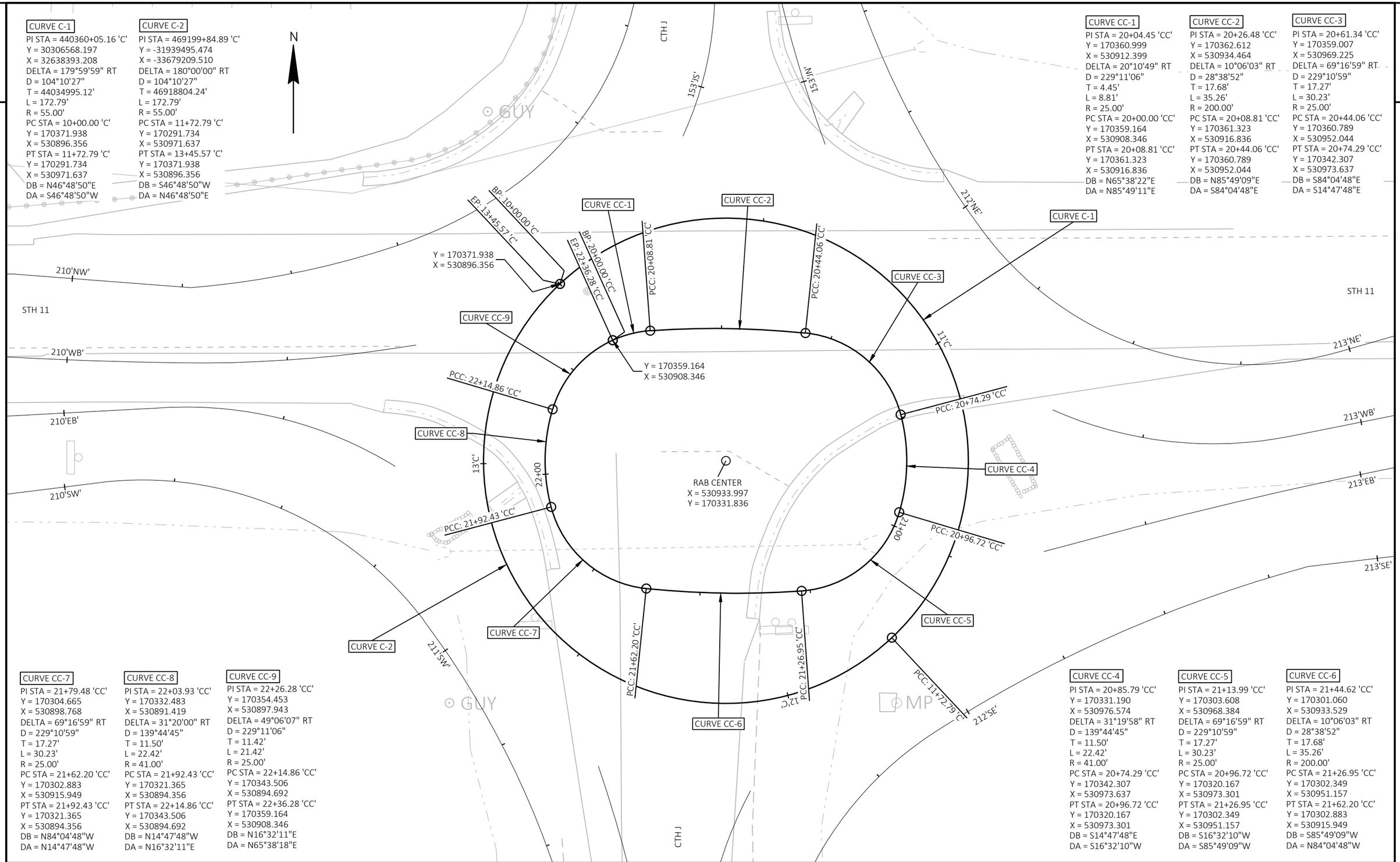
CURVE CC-5
PI STA = 21+13.99 'CC'
Y = 170303.608
X = 530968.384
DELTA = 69°16'59" RT
D = 229°10'59"
T = 17.27'
L = 30.23'
R = 25.00'
PC STA = 20+96.72 'CC'
Y = 170320.167
X = 530973.301
PT STA = 21+26.95 'CC'
Y = 170302.349
X = 530951.157
DB = S16°32'10"W
DA = S85°49'09"W

CURVE CC-6
PI STA = 21+44.62 'CC'
Y = 170301.060
X = 530933.529
DELTA = 10°06'03" RT
D = 28°38'52"
T = 17.68'
L = 35.26'
R = 200.00'
PC STA = 21+26.95 'CC'
Y = 170302.349
X = 530951.157
PT STA = 21+62.20 'CC'
Y = 170302.883
X = 530915.949
DB = S85°49'09"W
DA = N84°04'48"W

CURVE CC-7
PI STA = 21+79.48 'CC'
Y = 170304.665
X = 530898.768
DELTA = 69°16'59" RT
D = 229°10'59"
T = 17.27'
L = 30.23'
R = 25.00'
PC STA = 21+62.20 'CC'
Y = 170302.883
X = 530915.949
PT STA = 21+92.43 'CC'
Y = 170321.365
X = 530894.356
DB = N84°04'48"W
DA = N14°47'48"W

CURVE CC-8
PI STA = 22+03.93 'CC'
Y = 170332.483
X = 530891.419
DELTA = 31°20'00" RT
D = 139°44'45"
T = 11.50'
L = 22.42'
R = 41.00'
PC STA = 21+92.43 'CC'
Y = 170321.365
X = 530894.356
PT STA = 22+14.86 'CC'
Y = 170343.506
X = 530894.692
DB = N14°47'48"W
DA = N16°32'11"E

CURVE CC-9
PI STA = 22+26.28 'CC'
Y = 170354.453
X = 530897.943
DELTA = 49°06'07" RT
D = 229°11'06"
T = 11.42'
L = 21.42'
R = 25.00'
PC STA = 22+14.86 'CC'
Y = 170343.506
X = 530894.692
PT STA = 22+36.28 'CC'
Y = 170359.164
X = 530908.346
DB = N16°32'11"E
DA = N65°38'18"E



PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	ALIGNMENT DIAGRAM	SHEET 61	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\027203-AD.DWG
LAYOUT NAME - RAB-1
PLOT DATE : 6/27/2023 3:22 PM
PLOT BY : MUENCH, DOUGLAS
PLOT NAME :
PLOT SCALE : 1 IN:20 FT

CURVE NW-1
PI STA = 209+73.99 'NW'
Y = 170374.022
X = 530759.796
DELTA = 2°24'27" LT
D = 9°37'46"
T = 12.50'
L = 25.00'
R = 595.00'
PC STA = 209+61.49 'NW'
Y = 170374.374
X = 530747.298
PT STA = 209+86.49 'NW'
Y = 170374.195
X = 530772.297
DB = S88°23'06"E
DA = N89°12'27"E

CURVE NW-2
PI STA = 210+50.79 'NW'
Y = 170373.038
X = 530836.401
DELTA = 13°35'06" LT
D = 28°38'52"
T = 23.82'
L = 47.42'
R = 200.00'
PC STA = 210+26.97 'NW'
Y = 170371.108
X = 530812.657
PT STA = 210+74.39 'NW'
Y = 170380.490
X = 530859.027
DB = N85°21'18"E
DA = N71°46'12"E

CURVE NW-3
PI STA = 210+88.89 'NW'
Y = 170385.027
X = 530872.803
DELTA = 16°30'17" LT
D = 57°17'45"
T = 14.50'
L = 28.81'
R = 100.00'
PC STA = 210+74.39 'NW'
Y = 170380.490
X = 530859.027
PT STA = 211+03.20 'NW'
Y = 170393.291
X = 530884.722
DB = N71°46'12"E
DA = N55°15'55"E

CURVE NW-4
PI STA = 211+50.18 'NW'
Y = 170420.062
X = 530923.334
DELTA = 67°44'24" LT
D = 81°51'04"
T = 46.98'
L = 82.76'
R = 70.00'
PC STA = 211+03.20 'NW'
Y = 170393.291
X = 530884.722
PT STA = 211+85.96 'NW'
Y = 170465.938
X = 530913.185
DB = N55°15'55"E
DA = N12°28'29"W

CURVE NE-1
PI STA = 210+71.39 'NE'
Y = 170510.971
X = 530953.310
DELTA = 2°24'27" LT
D = 9°37'46"
T = 12.50'
L = 25.00'
R = 595.00'
PC STA = 210+58.89 'NE'
Y = 170523.390
X = 530951.859
PT STA = 210+83.89 'NE'
Y = 170498.625
X = 530955.281
DB = S06°39'53"E
DA = S09°04'21"E

CURVE NE-2
PI STA = 211+66.08 'NE'
Y = 170417.590
X = 530967.368
DELTA = 23°33'33" LT
D = 28°38'52"
T = 41.71'
L = 82.24'
R = 200.00'
PC STA = 211+24.37 'NE'
Y = 170458.241
X = 530958.039
PT STA = 212+06.61 'NE'
Y = 170384.056
X = 530992.168
DB = S12°55'30"E
DA = S36°29'03"E

CURVE NE-3
PI STA = 212+58.68 'NE'
Y = 170342.190
X = 531023.130
DELTA = 69°32'36" LT
D = 76°23'40"
T = 52.07'
L = 91.03'
R = 75.00'
PC STA = 212+06.61 'NE'
Y = 170384.056
X = 530992.168
PT STA = 212+97.64 'NE'
Y = 170356.566
X = 531073.177
DB = S36°29'03"E
DA = N73°58'21"E

CURVE SW-1
PI STA = 210+54.10 'SW'
Y = 170332.528
X = 530837.722
DELTA = 60°04'55" RT
D = 71°37'11"
T = 46.26'
L = 83.89'
R = 80.00'
PC STA = 210+07.83 'SW'
Y = 170326.870
X = 530791.805
PT STA = 210+91.72 'SW'
Y = 170295.552
X = 530865.529
DB = N82°58'28"E
DA = S36°56'36"E

CURVE SW-2
PI STA = 211+43.31 'SW'
Y = 170254.319
X = 530896.537
DELTA = 28°55'45" RT
D = 28°38'52"
T = 51.59'
L = 100.98'
R = 200.00'
PC STA = 210+91.72 'SW'
Y = 170295.552
X = 530865.529
PT STA = 211+92.70 'SW'
Y = 170203.231
X = 530903.729
DB = S36°56'36"E
DA = S08°00'51"E

CURVE SW-3
PI STA = 212+45.68 'SW'
Y = 170150.290
X = 530903.928
DELTA = 2°24'27" RT
D = 9°37'46"
T = 12.50'
L = 25.00'
R = 595.00'
PC STA = 212+33.18 'SW'
Y = 170162.759
X = 530903.020
PT STA = 212+58.18 'SW'
Y = 170137.793
X = 530904.310
DB = S04°09'42"E
DA = S01°45'15"E

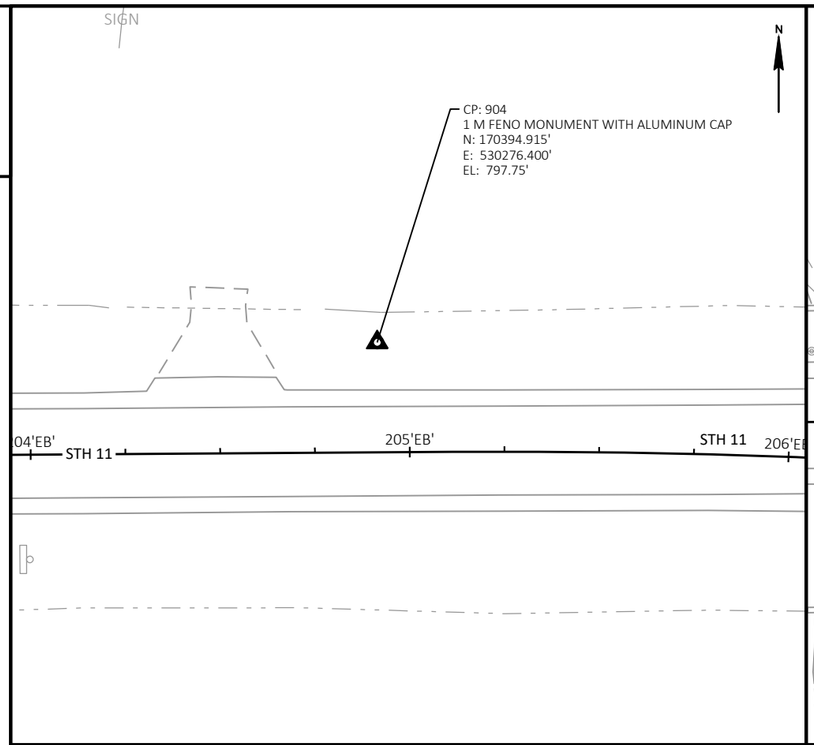
CURVE SE-1
PI STA = 211+50.92 'SE'
Y = 170242.106
X = 530937.546
DELTA = 66°12'32" RT
D = 71°37'11"
T = 52.16'
L = 92.44'
R = 80.00'
PC STA = 210+98.76 'SE'
Y = 170190.586
X = 530945.695
PT STA = 211+91.20 'SE'
Y = 170270.346
X = 530981.400
DB = N08°59'19"W
DA = N57°13'13"E

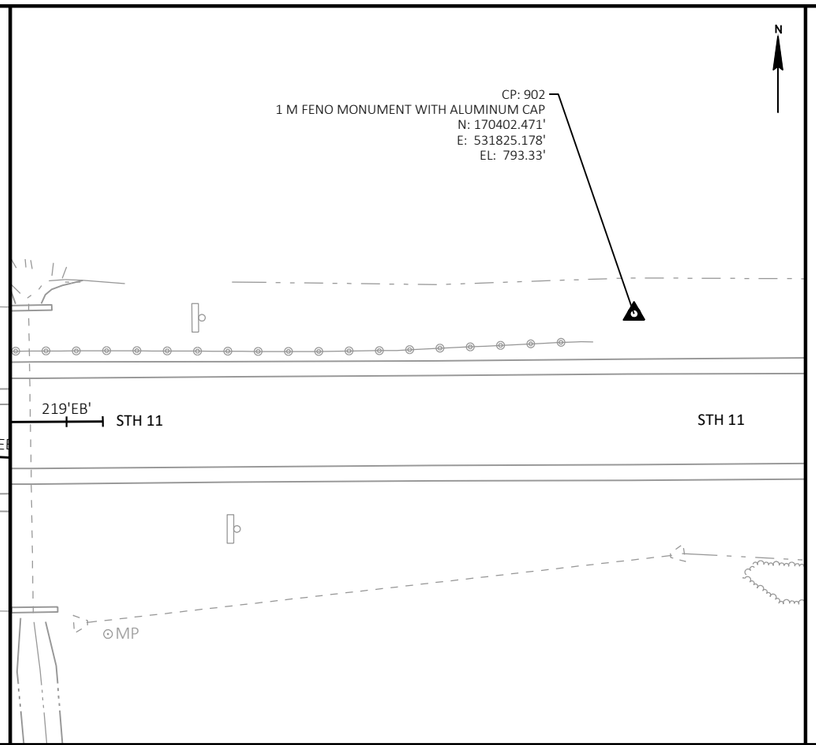
CURVE SE-2
PI STA = 212+38.05 'SE'
Y = 170295.710
X = 531020.789
DELTA = 17°45'06" RT
D = 19°05'55"
T = 46.85'
L = 92.95'
R = 300.00'
PC STA = 211+91.20 'SE'
Y = 170270.346
X = 530981.400
PT STA = 212+84.15 'SE'
Y = 170307.858
X = 531066.035
DB = N57°13'13"E
DA = N74°58'19"E

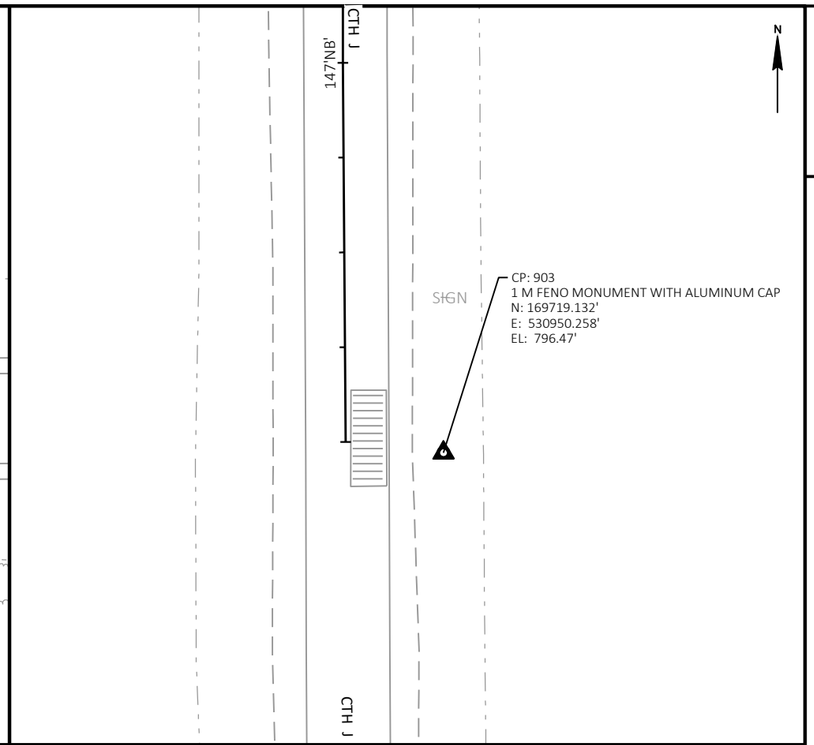
CURVE SE-3
PI STA = 213+37.06 'SE'
Y = 170315.375
X = 531118.358
DELTA = 1°26'23" RT
D = 5°45'30"
T = 12.50'
L = 25.00'
R = 995.00'
PC STA = 213+24.56 'SE'
Y = 170312.622
X = 531106.164
PT STA = 213+49.56 'SE'
Y = 170317.822
X = 531130.618
DB = N77°16'32"E
DA = N78°42'54"E

PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	ALIGNMENT DIAGRAM	SHEET 62	E

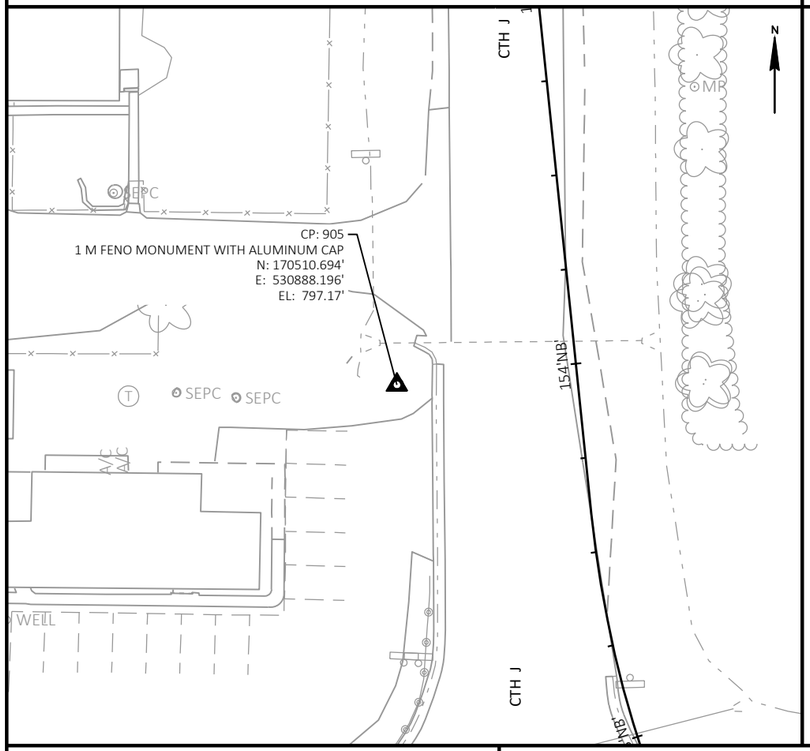
FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\027204-AD.DWG
LAYOUT NAME - RADIUS RETURNS
PLOT DATE : 6/27/2023 3:24 PM
PLOT BY : MUENCH, DOUGLAS
PLOT NAME :
PLOT SCALE : 1 IN:50 FT

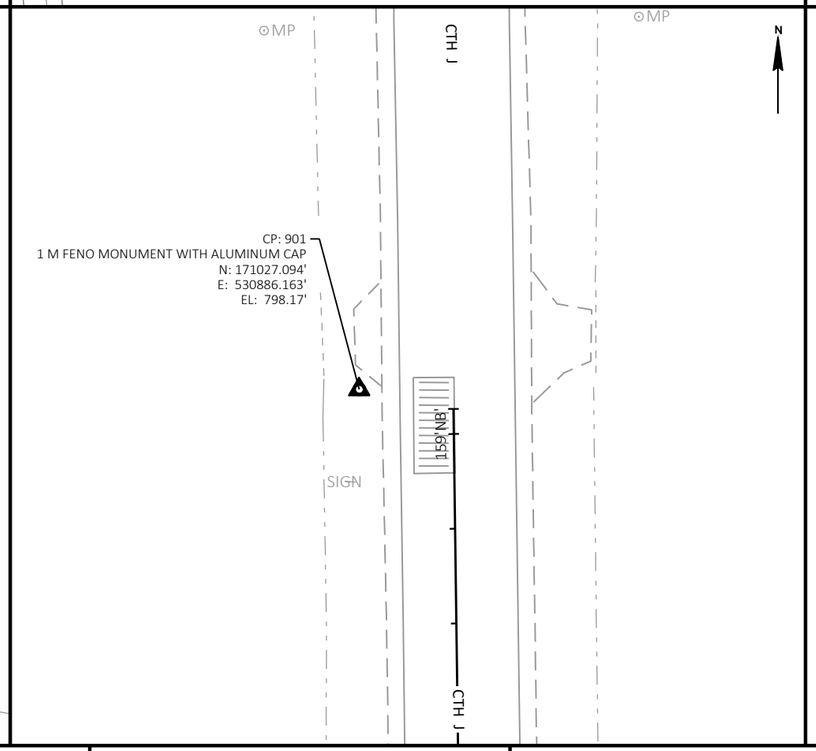






2





PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	ALIGNMENT DIAGRAM - SURVEY CONTROL	SHEET 63	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\027205-AD-CP.DWG
LAYOUT NAME - 01
PLOT DATE : 10/19/2023 9:00 AM
PLOT BY : MUENCH, DOUGLAS
PLOT NAME :
PLOT SCALE : 1 IN:40 FT
WISDOT/CADDS SHEET 42

Estimate Of Quantities

1320-07-73

Line	Item	Item Description	Unit	Total	Qty
0002	203.0100	Removing Small Pipe Culverts	EACH	5.000	5.000
0004	204.0110	Removing Asphaltic Surface	SY	76.000	76.000
0006	204.0150	Removing Curb & Gutter	LF	339.000	339.000
0008	204.0165	Removing Guardrail	LF	191.000	191.000
0010	204.0210	Removing Manholes	EACH	1.000	1.000
0012	204.0245	Removing Storm Sewer (size) 01. 18-INCH	LF	232.000	232.000
0014	204.9090.S	Removing (item description) 01. Culvert Pipe	LF	70.000	70.000
0016	205.0100	Excavation Common	CY	12,762.000	12,762.000
0018	213.0100	Finishing Roadway (project) 01. 1320-07-73	EACH	1.000	1.000
0020	305.0110	Base Aggregate Dense 3/4-Inch	TON	322.000	322.000
0022	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	10,357.000	10,357.000
0024	312.0110	Select Crushed Material	TON	10,370.000	10,370.000
0026	405.0100	Coloring Concrete WisDOT Red	CY	169.000	169.000
0028	415.2010	Concrete Truck Apron 12-inch	SY	505.000	505.000
0030	455.0605	Tack Coat	GAL	1,187.000	1,187.000
0032	460.2000	Incentive Density HMA Pavement	DOL	2,410.000	2,410.000
0034	460.6223	HMA Pavement 3 MT 58-28 S	TON	2,850.000	2,850.000
0036	460.6424	HMA Pavement 4 MT 58-28 H	TON	911.000	911.000
0038	465.0120	Asphaltic Surface Driveways and Field Entrances	TON	17.000	17.000
0040	465.0315	Asphaltic Flumes	SY	88.000	88.000
0042	520.1018	Apron Endwalls for Culvert Pipe 18-Inch	EACH	8.000	8.000
0044	520.1024	Apron Endwalls for Culvert Pipe 24-Inch	EACH	4.000	4.000
0046	520.1030	Apron Endwalls for Culvert Pipe 30-Inch	EACH	2.000	2.000
0048	520.3318	Culvert Pipe Class III-A 18-Inch	LF	74.000	74.000
0050	520.3324	Culvert Pipe Class III-A 24-Inch	LF	50.000	50.000
0052	520.3330	Culvert Pipe Class III-A 30-Inch	LF	146.000	146.000
0054	521.1036	Apron Endwalls for Culvert Pipe Steel 36-Inch	EACH	1.000	1.000
0056	522.2419	Culvert Pipe Reinforced Concrete Horizontal Elliptical Class HE-IV 19x30-Inch	LF	62.000	62.000
0058	522.2619	Apron Endwalls for Culvert Pipe Reinforced Concrete Horizontal Elliptical 19x30-Inch	EACH	2.000	2.000
0060	601.0405	Concrete Curb & Gutter 18-Inch Type A	LF	230.000	230.000
0062	601.0411	Concrete Curb & Gutter 30-Inch Type D	LF	1,818.000	1,818.000
0064	601.0551	Concrete Curb & Gutter 4-Inch Sloped 36-Inch Type A	LF	143.000	143.000
0066	601.0553	Concrete Curb & Gutter 4-Inch Sloped 36-Inch Type D	LF	1,128.000	1,128.000
0068	601.0580	Concrete Curb & Gutter 4-Inch Sloped 36-Inch Type R	LF	333.000	333.000
0070	601.0600	Concrete Curb Pedestrian	LF	96.000	96.000
0072	602.0405	Concrete Sidewalk 4-Inch	SF	8,310.000	8,310.000
0074	602.0410	Concrete Sidewalk 5-Inch	SF	5,560.000	5,560.000
0076	602.0505	Curb Ramp Detectable Warning Field Yellow	SF	320.000	320.000
0078	602.0810	Concrete Driveway 6-Inch	SY	42.000	42.000
0080	606.0100	Riprap Light	CY	40.000	40.000
0082	606.0200	Riprap Medium	CY	46.000	46.000
0084	608.3012	Storm Sewer Pipe Class III-A 12-Inch	LF	100.000	100.000
0086	608.3018	Storm Sewer Pipe Class III-A 18-Inch	LF	151.000	151.000
0088	608.3024	Storm Sewer Pipe Class III-A 24-Inch	LF	234.000	234.000
0090	611.0530	Manhole Covers Type J	EACH	2.000	2.000
0092	611.0624	Inlet Covers Type H	EACH	2.000	2.000
0094	611.0627	Inlet Covers Type HM	EACH	4.000	4.000
0096	611.0636	Inlet Covers Type HM-S	EACH	2.000	2.000
0098	611.0652	Inlet Covers Type T	EACH	2.000	2.000
0100	611.2005	Manholes 5-FT Diameter	EACH	2.000	2.000

1320-07-73

Line	Item	Item Description	Unit	Total	Qty
0102	611.3004	Inlets 4-FT Diameter	EACH	1.000	1.000
0104	611.3225	Inlets 2x2.5-FT	EACH	2.000	2.000
0106	611.3230	Inlets 2x3-FT	EACH	7.000	7.000
0108	612.0106	Pipe Underdrain 6-Inch	LF	315.000	315.000
0110	616.0700.S	Fence Safety	LF	180.000	180.000
0112	618.0100	Maintenance and Repair of Haul Roads (project) 01. 1320-07-73	EACH	1.000	1.000
0114	619.1000	Mobilization	EACH	1.000	1.000
0116	620.0300	Concrete Median Sloped Nose	SF	346.000	346.000
0118	624.0100	Water	MGAL	103.000	103.000
0120	625.0500	Salvaged Topsoil	SY	15,230.000	15,230.000
0122	628.1504	Silt Fence	LF	570.000	570.000
0124	628.1520	Silt Fence Maintenance	LF	1,140.000	1,140.000
0126	628.1905	Mobilizations Erosion Control	EACH	3.000	3.000
0128	628.1910	Mobilizations Emergency Erosion Control	EACH	3.000	3.000
0130	628.2004	Erosion Mat Class I Type B	SY	14,030.000	14,030.000
0132	628.2006	Erosion Mat Urban Class I Type A	SY	1,200.000	1,200.000
0134	628.7005	Inlet Protection Type A	EACH	12.000	12.000
0136	628.7015	Inlet Protection Type C	EACH	12.000	12.000
0138	628.7555	Culvert Pipe Checks	EACH	32.000	32.000
0140	628.7570	Rock Bags	EACH	35.000	35.000
0142	629.0210	Fertilizer Type B	CWT	11.000	11.000
0144	630.0130	Seeding Mixture No. 30	LB	286.000	286.000
0146	630.0140	Seeding Mixture No. 40	LB	21.000	21.000
0148	630.0200	Seeding Temporary	LB	461.000	461.000
0150	630.0500	Seed Water	MGAL	342.000	342.000
0152	633.5200	Markers Culvert End	EACH	13.000	13.000
0154	634.0614	Posts Wood 4x6-Inch X 14-FT	EACH	2.000	2.000
0156	634.0616	Posts Wood 4x6-Inch X 16-FT	EACH	40.000	40.000
0158	634.0814	Posts Tubular Steel 2x2-Inch X 14-FT	EACH	10.000	10.000
0160	634.0816	Posts Tubular Steel 2x2-Inch X 16-FT	EACH	12.000	12.000
0162	637.2210	Signs Type II Reflective H	SF	555.470	555.470
0164	637.2230	Signs Type II Reflective F	SF	49.500	49.500
0166	638.2602	Removing Signs Type II	EACH	23.000	23.000
0168	638.3000	Removing Small Sign Supports	EACH	23.000	23.000
0170	642.5201	Field Office Type C	EACH	1.000	1.000
0172	643.0300	Traffic Control Drums	DAY	2,320.000	2,320.000
0174	643.0420	Traffic Control Barricades Type III	DAY	3,060.000	3,060.000
0176	643.0705	Traffic Control Warning Lights Type A	DAY	4,680.000	4,680.000
0178	643.0900	Traffic Control Signs	DAY	17,370.000	17,370.000
0180	643.0920	Traffic Control Covering Signs Type II	EACH	13.000	13.000
0182	643.1050	Traffic Control Signs PCMS	DAY	28.000	28.000
0184	643.5000	Traffic Control	EACH	1.000	1.000
0186	645.0120	Geotextile Type HR	SY	144.000	144.000
0188	645.0130	Geotextile Type R	SY	138.000	138.000
0190	646.1020	Marking Line Epoxy 4-Inch	LF	4,213.000	4,213.000
0192	646.2020	Marking Line Epoxy 6-Inch	LF	2,926.000	2,926.000
0194	646.2040	Marking Line Grooved Wet Ref Epoxy 6-Inch	LF	2,036.000	2,036.000
0196	646.4020	Marking Line Epoxy 10-Inch	LF	243.000	243.000
0198	646.6320	Marking Dotted Extension Epoxy 18-Inch	LF	142.000	142.000
0200	646.7120	Marking Diagonal Epoxy 12-Inch	LF	233.000	233.000

Estimate Of Quantities

1320-07-73

Line	Item	Item Description	Unit	Total	Qty
0202	646.7420	Marking Crosswalk Epoxy Transverse Line 6-Inch	LF	297.000	297.000
0204	646.8120	Marking Curb Epoxy	LF	60.000	60.000
0206	646.8220	Marking Island Nose Epoxy	EACH	6.000	6.000
0208	650.4000	Construction Staking Storm Sewer	EACH	18.000	18.000
0210	650.4500	Construction Staking Subgrade	LF	4,255.000	4,255.000
0212	650.5000	Construction Staking Base	LF	4,255.000	4,255.000
0214	650.5500	Construction Staking Curb Gutter and Curb & Gutter	LF	3,653.000	3,653.000
0216	650.6000	Construction Staking Pipe Culverts	EACH	6.000	6.000
0218	650.7000	Construction Staking Concrete Pavement	LF	466.000	466.000
0220	650.9000	Construction Staking Curb Ramps	EACH	16.000	16.000
0222	650.9500	Construction Staking Sidewalk (project) 01. 1320-07-73	EACH	1.000	1.000
0224	650.9911	Construction Staking Supplemental Control (project) 01. 1320-07-73	EACH	1.000	1.000
0226	650.9920	Construction Staking Slope Stakes	LF	3,909.000	3,909.000
0228	652.0225	Conduit Rigid Nonmetallic Schedule 40 2-Inch	LF	1,350.000	1,350.000
0230	652.0615	Conduit Special 3-Inch	LF	230.000	230.000
0232	653.0140	Pull Boxes Steel 24x42-Inch	EACH	8.000	8.000
0234	654.0105	Concrete Bases Type 5	EACH	14.000	14.000
0236	654.0230	Concrete Control Cabinet Bases Type L30	EACH	1.000	1.000
0238	655.0610	Electrical Wire Lighting 12 AWG	LF	700.000	700.000
0240	655.0615	Electrical Wire Lighting 10 AWG	LF	1,335.000	1,335.000
0242	655.0620	Electrical Wire Lighting 8 AWG	LF	3,220.000	3,220.000
0244	655.0640	Electrical Wire Lighting 1 AWG	LF	18.000	18.000
0246	656.0201	Electrical Service Meter Breaker Pedestal (location) 01. HL-51-CJ	EACH	1.000	1.000
0248	657.0255	Transformer Bases Breakaway 11 1/2-Inch Bolt Circle	EACH	14.000	14.000
0250	657.0322	Poles Type 5-Aluminum	EACH	14.000	14.000
0252	657.0610	Luminaire Arms Single Member 4 1/2-Inch Clamp 6-FT	EACH	14.000	14.000
0254	659.1115	Luminaires Utility LED A	EACH	14.000	14.000
0256	659.2130	Lighting Control Cabinets 120/240 30-Inch	EACH	1.000	1.000
0258	690.0150	Sawing Asphalt	LF	294.000	294.000
0260	ASP.1T0A	On-the-Job Training Apprentice at $5.00/HR	HRS	800.000	800.000
0262	ASP.1T0G	On-the-Job Training Graduate at $5.00/HR	HRS	900.000	900.000
0264	SPV.0060	Special 01. Lighting System Integrator (1320-07-73)	EACH	1.000	1.000
0266	SPV.0060	Special 02. Lighting System Survey (1320-07-73)	EACH	1.000	1.000
0268	SPV.0060	Special 03. Section Corner Monuments	EACH	1.000	1.000
0270	SPV.0090	Special 01. Concrete Casing Pipe 12-Inch	LF	176.000	176.000

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REMOVAL ITEMS

CATEGORY	STATION	TO	STATION	LOCATION	203.0100 REMOVING SMALL PIPE CULVERTS EACH	204.0110 REMOVING ASPHALTIC SURFACE SY	204.0150 REMOVING CURB & GUTTER LF	204.0165 REMOVING GUARDRAIL LF	204.0210 REMOVING MANHOLES EACH	204.0245.01 REMOVING STORM SEWER (SIZE) (01. 18-INCH) LF	204.9090.S.01 REMOVING (ITEM DESCRIPTION) (01. CULVERT PIPE) LF	REMARKS
0010	206+87 'EB'	-	207+16 'EB'	STH 11 RT	1	-	-	-	-	-	-	28' - 24-INCH CPCS
	149+33 'JN'	-	149+55 'JN'	CTH J RT	1	-	-	-	-	-	-	22' - 15-INCH CPCS
	151+84 'JN'	-	151+96 'JN'	CTH J	1	-	-	-	-	-	-	94' - 24-INCH CPRC
	154+04 'JN'	-	154+11 'JN'	CTH J	1	-	-	-	-	-	-	70' - 24-INCH CPRC
	154+19 'JS'	-	154+48 'JS'	CTH J LT	1	-	-	-	-	-	-	30' - 12-INCH CPCS
	205+11 'EB'	-	217+76 'EB'	STH 11 RT	-	-	134	-	-	-	70	36-INCH CPCS
	205+11 'WB'	-	217+76 'WB'	STH 11 LT	-	76	205	191	1	232	-	
				TOTAL 0010	5	76	339	191	1	232	70	

EARTHWORK SUMMARY

CATEGORY 0010

FROM/TO STATION	LOCATION	205.0100 COMMON EXCAVATION (1) CUT (2)	205.0100 COMMON EXCAVATION (1) EBS EXCAVATION (3)	SALVAGED/UNUSABLE PAVEMENT MATERIAL (4)	AVAILABLE MATERIAL (5)	UNEXPANDED FILL	EXPANDED FILL (13) FACTOR 1.25	MASS ORDINATE +/- (14)	WASTE	208.0100 BORROW	COMMENT
205+11'EB' - 217+76'EB'	EB - STH 11	3,221	0	186	3,035	1,709	2,136	899	899	0	
205+11'WB' - 217+76'WB'	WB - STH 11	3,638	0	474	3,164	182	228	2,937	2,937	0	
147+00'JN' - 156+86'JN'	NB - CTH J	2,463	0	142	2,321	681	851	1,470	1,470	0	
147+00'JS' - 156+86'JS'	SB - CTH J	2,260	0	191	2,069	223	279	1,790	1,790	0	
10+00'C' - 13+46'C'	RAB	1,180	0	270	910	2,144	2,680	-1,770	0	0	
PROJECT TOTALS		12,762	0	1,263	11,499	4,939	6,174	5,325	7,095	0	

NOTES:

(1) COMMON EXCAVATION IS THE SUM OF THE CUT AND EBS EXCAVATION COLUMNS. ITEM NUMBER 205.0100

(2) SALVAGED/UNSUABLE PAVEMENT MATERIAL IS INCLUDED IN CUT.

(3) EBS EXCAVATION TO BE BACKFILLED WITH SELECT BORROW MATERIAL. NOTE: THIS IS DESIGNERS CHOICE, CAN BE BACKFILLED WITH BORROW, OR CUT AS WELL.

(4) SALVAGED/UNUSABLE PAVEMENT MATERIAL

(5) AVAILABLE MATERIAL = CUT - SALVAGED/UNUSUABLE PAVEMENT MATERIAL

(13) EXPANDED FILL FACTOR = 1.25

(14) THE MASS ORDINATE + OR - QTY CALCULATED FOR THE DIVISION. PLUS QUANTITY INDICATES AN EXCESS OF MATERIAL WITHIN THE DIVISION. MINUS INDICATES A SHORTAGE OF MATERIAL WITHIN THE DIVISION.

ALL QUANTITIES CATEGORY 0010 UNLESS OTHERWISE NOTED

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BASE AGGREGATE ITEMS

CATEGORY	STATION	TO	STATION	LOCATION	305.0110 BASE AGGREGATE DENSE 3/4-INCH TON	* 305.0120 BASE AGGREGATE DENSE 1 1/4-INCH TON	312.0110 SELECT CRUSHED MATERIAL TON	624.0100 WATER MGAL
0010	205+11 'EB'	-	217+76 'EB'	STH 11 RT	94	2,730	2,860	28
	205+11 'WB'	-	217+76 'WB'	STH 11 LT	101	2,490	2,580	26
	147+00 'JN'	-	156+86 'JN'	CTH J RT	62	2,070	2,190	21
	147+00 'JS'	-	156+86 'JS'	CTH J LT	65	1,790	1,820	19
	10+00 'C'	-	13+46 'C'	ROUNDABOUT	-	890	920	9
				TOTAL 0010	322	9,970	10,370	103

* ADDITIONAL QUANTITIES LISTED ELSEWHERE IN PLAN

DRIVEWAY ITEMS

CATEGORY	STATION	TO	STATION	LOCATION	* 305.0120 BASE AGGREGATE DENSE 1 1/4-INCH TON	465.0120 ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES TON	602.0810 CONCRETE DRIVEWAY 6-INCH SY
0010	153+50 'JS'	-	153+95 'JS'	CTH J LT	23	5	42
	154+11 'JS'	-	154+55 'JS'	CTH J LT	22	12	-
				TOTAL 0010	45	17	42

* ADDITIONAL QUANTITIES LISTED ELSEWHERE IN PLAN
** ASSUMED ASPHALT AT 112 LBS/SY/IN

CONCRETE TRUCK APRON ITEMS

CATEGORY	STATION	TO	STATION	LOCATION	405.0100 COLORING CONCRETE WISDOT RED CY	415.2010 CONCRETE TRUCK APRON 12-INCH SY
0010	210+12 'SW'	-	210+73 'SW'	SW QUAD	6	16
	212+40 'SW'	-	213+00 'NE'	NE QUAD	6	17
	10+00 'C '	-	13+46 'C'	ROUNDABOUT	157	472
				TOTAL 0010	169	505

HMA PAVEMENT ITEMS

CATEGORY	STATION	TO	STATION	LOCATION	LOWER LAYER THICKNESS IN	MIDDLE LAYER THICKNESS IN	UPPER LAYER THICKNESS IN	455.0605 TACK COAT GAL	460.6223 HMA PAVEMENT 3 MT 58-28 S TON	460.6424 HMA PAVEMENT 4 MT 58-28 H TON
0010	205+11 'EB'	-	210+78 'EB'	STH 11, WEST LEG	3.00	3.00	1.75	269	645	188
	212+26 'EB'	-	217+76 'EB'	STH 11, EAST LEG	3.00	3.00	1.75	257	618	180
	147+00 'JN'	-	151+37 'JN'	CTH J, SOUTH LEG	3.00	3.00	1.75	208	500	146
	152+85 'JN'	-	156+86 'JN'	CTH J, NORTH LEG	3.00	3.00	1.75	210	504	147
	10+00 'C'	-	13+46 'C'	ROUNDABOUT	3.00	3.00	1.75	127	305	89
	205+11 'EB'	-	210+78 'EB'	STH 11, WEST LEG SHLDR	3.00	-	1.75	36	86	50
	212+26 'EB'	-	217+76 'EB'	STH 11, EAST LEG SHLDR	3.00	-	1.75	33	79	46
	147+00 'JN'	-	151+37 'JN'	CTH J, SOUTH LEG SHLDR	3.00	-	1.75	25	61	35
	152+85 'JN'	-	156+86 'JN'	CTH J, NORTH LEG SHLDR	3.00	-	1.75	22	52	30
				TOTAL 0010				1,187	2,850	911

NOTES:
* TACK COAT APPLICATION RATE = 0.07 GAL/SY
** ASSUMED ASPHALT AT 112 LBS/SY/IN

ASPHALTIC FLUMES

CATEGORY	STATION	TO	STATION	LOCATION	465.0315 ASPHALTIC FLUMES SY
0010	207+82 'EB'	-	210+78 'EB'	STH 11, WEST LEG	22
	212+26 'EB'	-	215+74 'EB'	STH 11, EAST LEG	22
	149+43 'JN'	-	151+37 'JN'	CTH J, SOUTH LEG	22
	152+85 'JN'	-	154+83 'JN'	CTH J, NORTH LEG	22
				TOTAL 0010	88

ALL QUANTITIES CATEGORY 0010 UNLESS OTHERWISE NOTED

CULVERT ITEMS

***INLET STATION	***INLET OFFSET	***INLET ELEV.(FT)	***OUTLET STATION	***OUTLET OFFSET	***OUTLET ELEV.(FT)	SLOPE (%)	LOCATION	* 520.1018 APRON ENDWALLS FOR CULVERT PIPE 18-INCH EACH	* 520.1024 APRON ENDWALLS FOR CULVERT PIPE 24-INCH EACH	520.1030 APRON ENDWALLS FOR CULVERT PIPE 30-INCH EACH	520.3318 CULVERT PIPE CLASS III-A 18-INCH LF	520.3324 CULVERT PIPE CLASS III-A 24-INCH LF	520.3330 CULVERT PIPE CLASS III-A 30-INCH LF	521.1036 APRON ENDWALLS FOR CULVERT PIPE STEEL 36-INCH EACH	522.2419 CULVERT PIPE REINFORCED CONCRETE HORIZONTAL ELLIPTICAL CLASS HE-IV 19X30-INCH LF	522.2619 APRON ENDWALLS FOR CULVERT PIPE REINFORCED CONCRETE HORIZONTAL ELLIPTICAL 19X30-INCH EACH	* 633.5200 MARKERS CULVERT END EACH	SPV.0090.01 SPECIAL (01. CONCRETE CASING PIPE 12-INCH) LF	** JOINT TIES EACH
206+78 'EB'	47.1' RT	792.68	207+29 'EB'	47.0' RT	792.53	0.30	STH 11 RT	-	2	-	-	50	-	-	-	-	-	-	-
217+79 'EB'	46.9' RT	789.10	-	-	-	-	STH 11 RT	-	-	-	-	-	-	1	-	-	1	-	-
148+39 'JN'	39.4' RT	793.37	148+85 'JN'	41.1' RT	792.94	0.93	CTH J RT	2	-	-	46	-	-	-	-	-	-	-	-
151+41 'JS'	51.3' LT	791.32	151+37 'JN'	64.7' RT	790.88	0.30	CTH J	-	-	2	-	-	146	-	-	-	2	-	-
154+19 'JN'	34.5' LT	794.42	154+46 'JN'	34.5' LT	794.32	0.35	CTH J LT	2	-	-	28	-	-	-	-	-	-	-	-
154+74 'JS'	23.5' LT	794.25	154+74' JN'	25.8' RT	793.50	1.21	CTH J	-	-	-	-	-	-	-	62	2	2	-	12
150+39 'JS'	80' LT	787.88	150+32 'JS'	84' RT	787.70	0.10	CTH J	-	-	-	-	-	-	-	-	-	2	176	-
							TOTAL 0010	4	2	2	74	50	146	1	62	2	7	176	12

** NON-BID ITEM: FOR INFORMATION ONLY

*** PIPE INVERT AT END OF PIPE & STATION/OFFSET TO CENTER OF END OF PIPE

CONCRETE CURB & GUTTER

CATEGORY	STATION	TO	STATION	LOCATION	601.0405 CONCRETE CURB & GUTTER 18-INCH TYPE A LF	601.0411 CONCRETE CURB & GUTTER 30-INCH TYPE D LF	601.0551 CONCRETE CURB & GUTTER 4-INCH SLOPED 36-INCH TYPE A LF	601.0553 CONCRETE CURB & GUTTER 4-INCH SLOPED 36-INCH TYPE D LF	601.0580 CONCRETE CURB & GUTTER 4-INCH SLOPED 36-INCH TYPE R LF	620.0300 CONCRETE MEDIAN SLOPED NOSE SF
0010	207+82 'EB'	-	210+78 'EB'	STH 11, WEST LEG	-	209	-	393	-	68
	212+26 'EB'	-	215+74 'EB'	STH 11, EAST LEG	-	220	-	487	-	59
	149+43 'JN'	-	151+37 'JN'	CTH J, SOUTH LEG	-	215	-	193	-	59
	152+85 'JN'	-	154+83 'JN'	CTH J, NORTH LEG	-	134	-	55	-	160
	10+00 'C'	-	13+46 'C'	ROUNDABOUT	230	-	-	-	333	-
	209+48 'SW'	-	212+58 'SW'	SW QUAD	-	236	72	-	-	-
	210+38 'SE'	-	213+50 'SE'	SE QUAD	-	308	-	-	-	-
	210+59 'NE'	-	213+58 'NE'	NE QUAD	-	225	71	-	-	-
	209+62 'NW'	-	212+36 'NW'	NW QUAD	-	271	-	-	-	-
				TOTAL 0010	230	1,818	143	1,128	333	346

ALL QUANTITIES CATEGORY 0010 UNLESS OTHERWISE NOTED

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CONCRETE SIDEWALK ITEMS

CATEGORY	STATION	TO	STATION	LOCATION	* 305.0120 BASE AGGREGATE DENSE 1 1/4-INCH TON	601.0600 CONCRETE CURB PEDESTRIAN LF	602.0405 CONCRETE SIDEWALK 4-INCH SF	602.0410 CONCRETE SIDEWALK 5-INCH SF	602.0505 CURB RAMP DETECTABLE WARNING FIELD YELLOW SF
0010	207+82 'EB'	-	210+78 'EB'	STH 11, WEST LEG	80	19	3,130	110	40
	212+26 'EB'	-	215+74 'EB'	STH 11, EAST LEG	81	27	3,140	150	40
	149+43 'JN'	-	151+37 'JN'	CTH J, SOUTH LEG	38	26	1,390	140	40
	152+85 'JN'	-	154+83 'JN'	CTH J, NORTH LEG	19	24	650	130	40
	209+48 'SW'	-	212+58 'SW'	SW QUAD	32	-	-	1,310	40
	210+38 'SE'	-	213+50 'SE'	SE QUAD	34	-	-	1,370	40
	210+59 'NE'	-	213+58 'NE'	NE QUAD	31	-	-	1,240	40
	209+62 'NW'	-	212+36 'NW'	NW QUAD	27	-	-	1,110	40
				TOTAL 0010	342	96	8,310	5,560	320

*** ADDITIONAL QUANTITIES LISTED ELSEWHERE IN PLAN**

FENCE SAFETY

CATEGORY	STATION	TO	STATION	LOCATION	616.0700.S FENCE SAFETY LF
0010	209+77 'NW'	-	211+90 'NW'	NW QUAD	180
				TOTAL 0010	180

STORM SEWER PIPES

CATEGORY	FROM		TO	608.3012 STORM SEWER PIPE CLASS III-A 12-INCH LF	608.3018 STORM SEWER PIPE CLASS III-A 18-INCH LF	608.3024 STORM SEWER PIPE CLASS III-A 24-INCH LF	INLET ELEVATION	DISCHARGE ELEVATION	SLOPE FT/FT
0010	10	-	11	16	-	-	793.89	793.81	0.005
	11	-	12	-	32	-	793.31	793.15	0.005
	20	-	21	8	-	-	792.00	791.96	0.005
	21	-	22	-	32	-	791.46	791.30	0.005
	30	-	31	8	-	-	793.12	793.08	0.005
	31	-	32	-	34	-	792.58	792.41	0.005
	41	-	40	7	-	-	794.00	793.96	0.005
	40	-	42	41	-	-	793.86	793.66	0.005
	50	-	51	-	-	101	794.20	793.64	0.0055
	51	-	42	-	-	82	793.54	793.14	0.005
	42	-	52	-	-	51	793.04	792.88	0.0031
	60	-	61	20	-	-	793.62	793.52	0.005
	61	-	62	-	53	-	793.01	792.75	0.005
			TOTAL 0010	100	151	234			

PIPE UNDERDRAIN

	BEGIN				END					612.0106 PIPE UNDERDRAIN 6-INCH
CATEGORY	STATION	OFFSET	ELEV	STR NO.	STATION	OFFSET	ELEV	STR NO.	LOCATION	LF
0010	11+07 'C'	RT	794.54	60	10+86 'C'	RT	794.50	61	ROUNDABOUT	315
									TOTAL 0010	315

**** GEOTEXTILE TYPE DF SCHEDULE A AND BASE AGGREGATE OPEN-GRADED INCIDENTAL TO PIPE UNDERDRAIN ITEM**

ALL QUANTITIES CATEGORY 0010 UNLESS OTHERWISE NOTED

STORM SEWER STRUCTURES

STRUCTURE	** STATION	** OFFSET	* 520.1018 APRON ENDWALLS FOR CULVERT PIPE 18-INCH EACH	* 520.1024 APRON ENDWALLS FOR CULVERT PIPE 24-INCH EACH	611.0530 MANHOLE COVERS TYPE J EACH	611.0624 INLET COVERS TYPE H EACH	611.0627 INLET COVERS TYPE HM EACH	611.0636 INLET COVERS TYPE HM-S EACH	611.0652 INLET COVERS TYPE T EACH	611.2005 MANHOLES 5-FT DIAMETER EACH	611.3004 INLETS 4-FT DIAMETER EACH	611.3225 INLETS 2X2.5-FT EACH	611.3230 INLETS 2X3-FT EACH	* 633.5200 MARKERS CULVERT END EACH	RIM EL	INVERT EL	*** DEPTH FT
10	209+25 'WB'	1.5' RT	-	-	-	-	-	1	-	-	-	-	1	-	798.64	793.72	3.92
11	209+25 'EB'	1.5' LT	-	-	-	-	-	1	-	-	-	-	1	-	798.81	793.10	4.71
12	209+24 'EB'	30.5' RT	1	-	-	-	-	-	-	-	-	-	-	1	-	793.15	-
20	215+73 'WB'	3.2' RT	-	-	-	-	1	-	-	-	-	-	1	-	796.94	791.83	4.11
21	215+72 'EB'	1.5' LT	-	-	-	-	1	-	-	-	-	-	1	-	797.14	791.25	4.89
22	215+71 'EB'	30.5' RT	1	-	-	-	-	-	-	-	-	-	-	1	-	791.30	-
30	149+45 'JS'	1.5' RT	-	-	-	-	1	-	-	-	-	-	1	-	798.56	792.95	4.61
31	149+44 'JN'	3.2' LT	-	-	-	-	1	-	-	-	-	-	1	-	798.38	792.37	5.01
32	149+44 'JN'	30.9' RT	1	-	-	-	-	-	-	-	-	-	-	1	-	792.41	-
40	153+45 'JS'	1.5' RT	-	-	-	1	-	-	-	-	1	-	-	-	798.57	793.86	3.71
41	153+48 'JN'	3.8' LT	-	-	-	1	-	-	-	-	-	-	1	-	798.39	793.79	3.60
42	153+06 'JN'	5.8' LT	-	-	1	-	-	-	-	1	-	-	-	-	799.24	792.79	7.69
50	209+81 'WB'	40.9' LT	-	1	-	-	-	-	-	-	-	-	-	1	-	794.20	-
51	13+41 'C'	33' LT	-	-	1	-	-	-	-	1	-	-	-	-	799.53	793.23	5.06
52	153+13 'JN'	44.8' RT	-	1	-	-	-	-	-	-	-	-	-	1	-	792.88	-
60	11+07 'C'	1.7' RT	-	-	-	-	-	-	1	-	-	1	-	-	798.54	793.45	4.01
61	10+86 'C'	1.7' RT	-	-	-	-	-	-	1	-	-	1	-	-	798.50	792.85	4.57
62	10+88 'C'	51.2' LT	1	-	-	-	-	-	-	-	-	-	-	1	-	792.75	-
		TOTAL 0010	4	2	2	2	4	2	2	2	1	2	7	6			

*** ADDITIONAL QUANTITIES LISTED ELSEWHERE IN PLAN**
**** STATION & OFFSET ARE TO CENTER OF STRUCTURE**
***** DEPTH = RIM ELEV - STR INVERT - COVER HT - 6 IN ADJ RING HT**

EROSION CONTROL ITEMS

CATEGORY	STATION	TO	STATION	LOCATION	606.0100 RIPRAP LIGHT CY	606.0200 RIPRAP MEDIUM CY	628.1504 SILT FENCE LF	628.1520 SILT FENCE MAINTENANCE LF	628.2004 EROSION MAT CLASS I TYPE B SY	628.2006 EROSION MAT URBAN CLASS I TYPE A SY	628.7005 INLET PROTECTION TYPE A EACH	628.7015 INLET PROTECTION TYPE C EACH	628.7555 CULVERT PIPE CHECKS EACH	628.7570 ROCK BAGS EACH	645.0120 GEOTEXTILE TYPE HR SY	645.0130 GEOTEXTILE TYPE R SY
0010	207+82 'EB'	-	210+78 'EB'	STH 11, WEST LEG	9	11	-	-	2,690	-	2	2	6	-	34	39
	212+26 'EB'	-	215+74 'EB'	STH 11, EAST LEG	15	13	170	340	3,120	-	2	2	7	30	47	27
	149+43 'JN'	-	151+37 'JN'	CTH J, SOUTH LEG	9	8	-	-	1,940	-	2	2	7	-	29	41
	152+85 'JN'	-	154+83 'JN'	CTH J, NORTH LEG	7	14	300	600	960	-	2	2	7	-	34	31
	10+00 'C'	-	13+46 'C'	ROUNDABOUT	-	-	-	-	-	440	2	2	-	-	-	-
	209+48 'SW'	-	212+58 'SW'	SW QUAD	-	-	-	-	1,100	150	-	-	-	-	-	-
	209+62 'NW'	-	212+36 'NW'	NW QUAD	-	-	-	-	270	120	-	-	-	-	-	-
	210+59 'NE'	-	213+58 'NE'	NE QUAD	-	-	-	-	900	130	-	-	-	-	-	-
	210+38 'SE'	-	213+50 'SE'	SE QUAD	-	-	-	-	1,050	160	-	-	-	-	-	-
				UNDISTRIBUTED	-	-	100	200	2,000	200	2	2	5	5	-	-
				TOTAL 0010	40	46	570	1,140	14,030	1,200	12	12	32	35	144	138

ALL QUANTITIES CATEGORY 0010 UNLESS OTHERWISE NOTED

3

MOBILIZATIONS EROSION CONTROL

CATEGORY	LOCATION	628.1905 MOBILIZATIONS EROSION CONTROL EACH	628.1910 MOBILIZATIONS EMERGENCY EROSION CONTROL EACH
0010	PROJECT 1320-07-73	3	3
	TOTAL 0010	3	3

REMOVING SIGNS

CATEGORY	SIGN NUMBER	SIGN MESSAGE	638.2602 REMOVING SIGNS TYPE II EACH	638.3000 REMOVING SMALL SIGN SUPPORTS EACH
0010	R-10	N-COUNTY J / S-COUNTY J	1	1
	R-11	SPEED LIMIT 55	1	1
	R-12	WEST-11	1	1
	R-13	N-COUNTY J / S-COUNTY J	1	1
	R-20	N-COUNTY J / S-COUNTY J	1	1
	R-21	EAST 11	1	1
	R-22	SPEED LIMIT 55	1	1
	R-23	N-COUNTY J / S-COUNTY J	1	1
	R-24	NO PASSING ZONE	1	1
	R-30	DURAND AV	1	1
	R-31	JCT-11	1	1
	R-32	SPEED LIMIT 55	1	1
	R-33	WEIGHT LIMIT CLASS B	1	1
	R-34	SOUTH -COUNTY J	1	1
	R-35	STOP	1	1
	R-36	STOP/TWO W4-4C SIGNS	1	1
	R-37	STH-11	1	1
	R-40	STH-11	1	1
	R-41	STOP/TWO W4-4C SIGNS	1	1
	R-42	STOP	1	1
	R-43	NORTH- COUNTY J	1	1
	R-44	JCT-11	1	1
	R-45	DURAND AV	1	1
	TOTAL 0010		23	23

FINISHING ITEMS

CATEGORY	STATION	TO	STATION	LOCATION	625.0500 SALVAGED TOPSOIL SY	629.0210 FERTILIZER TYPE B CWT	630.0130 SEEDING MIXTURE NO. 30 LB	630.0140 SEEDING MIXTURE NO. 40 LB	630.0200 SEEDING TEMPORARY LB	630.0500 SEED WATER MGAL
0010	207+82 'EB'	-	210+78 'EB'	STH 11, WEST LEG	2,690	1.9	54	-	81	60
	212+26 'EB'	-	215+74 'EB'	STH 11, EAST LEG	3,120	2.2	63	-	95	70
	149+43 'JN'	-	151+37 'JN'	CTH J, SOUTH LEG	1,940	1.4	40	-	59	44
	152+85 'JN'	-	154+83 'JN'	CTH J, NORTH LEG	960	0.8	22	-	32	22
	10+00 'C'	-	13+46 'C'	ROUNDABOUT	440	0.3	-	8	12	10
	209+48 'SW'	-	212+58 'SW'	SW QUAD	1250	0.8	20	3	34	28
	209+62 'NW'	-	212+36 'NW'	NW QUAD	390	0.2	5	2	11	9
	210+59 'NE'	-	213+58 'NE'	NE QUAD	1,030	0.6	16	2	28	23
	210+38 'SE'	-	213+50 'SE'	SE QUAD	1,210	0.8	19	3	33	27
				UNDISTRIBUTED	2,200	2.0	47	3	76	49
				TOTAL 0010	15,230	11	286	21	461	342

SAWING ASPHALT

CATEGORY	STATION	TO	STATION	LOCATION	690.0150 SAWING ASPHALT LF
0010		205+11 'EB'		STH 11	32
		217+76 'EB'		STH 11	32
		147+00 'JN'		CTH J	22
	153+55 'JS'	-	153+90 'JS'	CTH J LT	35
	154+24 'JS'	-	154+42 'JS'	CTH J LT	20
		156+86 'JN'		CTH J	31
	209+84 'NW'	-	211+33 'NW'	NW QUAD	122
				TOTAL 0010	294

SECTION CORNER MONUMENTS

CATEGORY	LOCATION	SPV.0060.03 SPECIAL (03. SECTION CORNER MONUMENTS) EACH
0010	PROJECT 1320-07-73	1
	TOTAL 0010	1

ALL QUANTITIES CATEGORY 0010 UNLESS OTHERWISE NOTED

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PERMANENT SIGNING ITEMS - TYPE II

CATEGORY	SIGN NUMBER	SIGN CODE	SIZE (INCH)	SIZE (INCH)	DESCRIPTION	634.0614 POSTS WOOD 4X6-INCH X 14 FT EACH	634.0616 POSTS WOOD 4X6-INCH X 16-FT EACH	634.0814 POSTS TUBULAR STEEL 2X2-INCH X 14-FT EACH	634.0816 POSTS TUBULAR STEEL 2X2-INCH X 16-FT EACH	637.2210 SIGNS TYPE II REFLECTIVE H SF	637.2230 SIGNS TYPE II REFLECTIVE F SF
0010	01-01	J1-1	24	39	JCT	-	1	-	-	6.50	-
	01-02	W13-1	18	18	20 MPH	-	1	-	-	-	2.25
	01-03	W2-6	30	30	CIRCULAR INTERSECTION SIGN	-	-	-	-	-	6.25
	01-04	R2-1	24	30	SPEED LIMIT (55 MPH)	-	1	-	-	5.00	-
	01-05	W5-54	18	18	CLEARANCE MARKER	-	1	-	-	-	2.25
	01-06	R4-7	24	30	KEEP RIGHT	-	-	-	-	5.00	-
	01-07	J4-1	24	36	REASSURANCE ASSEMBLY	-	1	-	-	6.00	-
	01-08	D1-62	108	66	ROUNDABOUT DESTINATION	-	2	-	-	49.50	-
	01-09	R5-1	36	36	DO NOT ENTER	-	1	-	-	9.00	-
	01-10	W14-3	48	36	NO PASSING ZONE	-	1	-	-	5.56	-
	01-11	W6-3	36	36	TWO-WAY TRAFFIC	-	1	-	-	9.00	-
	02-01	R1-2	36	31	YIELD	-	-	1	-	3.88	-
	02-02	R1-2	36	31	YIELD	-	-	1	-	3.88	-
	02-03	D1-2	84	30	TWO DESTINATIONS	-	-	-	2	17.50	-
	02-04	R1-2	36	31	YIELD	-	-	1	-	3.88	-
	02-05	R1-2	36	31	YIELD	-	-	1	-	3.88	-
	02-06	D1-1	42	30	ONE DESTINATION	-	-	1	-	8.75	-
	02-07	R1-2	36	31	YIELD	-	-	1	-	3.88	-
	02-08	R1-2	36	31	YIELD	-	-	1	-	3.88	-
	02-09	D1-2	84	30	TWO DESTINATIONS	-	-	-	2	17.50	-
	02-10	R1-2	36	31	YIELD	-	-	1	-	3.88	-
	02-11	R1-2	36	31	YIELD	-	-	1	-	3.88	-
	02-12	D1-1	42	30	ONE DESTINATION	-	-	1	-	8.75	-
	02-13	R6-4B	60	24	ROUNDABOUT CHEVRON BANK	-	-	-	2	10.00	-
	02-14	R6-1R	36	12	ONE WAY RIGHT ARROW	-	-	-	-	3.00	-
	02-15	R6-4B	60	24	ROUNDABOUT CHEVRON BANK	-	-	-	2	10.00	-
	02-16	R6-1R	36	12	ONE WAY RIGHT ARROW	-	-	-	-	3.00	-
	02-17	R6-4B	60	24	ROUNDABOUT CHEVRON BANK	-	-	-	2	10.00	-
	02-18	R6-1R	36	12	ONE WAY RIGHT ARROW	-	-	-	-	3.00	-
	02-19	R6-4B	60	24	ROUNDABOUT CHEVRON BANK	-	-	-	2	10.00	-
	02-20	R6-1R	36	12	ONE WAY RIGHT ARROW	-	-	-	-	3.00	-
	02-21	R1-54	24	15	TO TRAFFIC FROM LEFT	-	-	-	-	2.50	-
	02-22	R1-54	24	15	TO TRAFFIC FROM LEFT	-	-	-	-	2.50	-
	02-23	R1-54	24	15	TO TRAFFIC FROM LEFT	-	-	-	-	2.50	-
	02-24	R1-54	24	15	TO TRAFFIC FROM LEFT	-	-	-	-	2.50	-
					PAGE 1/2 - SUBTOTALS	-	10	10	12	241.10	10.75

ALL QUANTITIES CATEGORY 0010 UNLESS OTHERWISE NOTED

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PERMANENT SIGNING ITEMS - TYPE II (CONTINUED)

CATEGORY	SIGN NUMBER	SIGN CODE	SIZE (INCH)	SIZE (INCH)	DESCRIPTION	634.0614 POSTS WOOD 4X6-INCH X 14-FT EACH	634.0616 POSTS WOOD 4X6-INCH X 16-FT EACH	634.0814 POSTS TUBULAR STEEL 2X2-INCH X 14-FT EACH	634.0816 POSTS TUBULAR STEEL 2X2-INCH X 16-FT EACH	637.2210 SIGNS TYPE II REFLECTIVE H SF	637.2230 SIGNS TYPE II REFLECTIVE F SF
0010	03-01	J4-1	24	36	REASSURANCE ASSEMBLY	-	1	-	-	6.00	-
	03-02	W5-54	18	18	CLEARANCE MARKER	-	1	-	-	-	2.25
	03-03	R4-7	24	30	KEEP RIGHT	-	-	-	-	5.00	-
	03-04	R2-1	24	30	SPEED LIMIT (55 MPH)	-	1	-	-	5.00	-
	03-05	W13-1	18	18	ADVISORY SPEED PLATE 20MPH	-	1	-	-	-	2.25
	03-06	W2-6	30	30	CIRCULAR INTERSECTION SIGN	-	-	-	-	-	6.25
	03-07	J1-1	24	39	JCT	-	1	-	-	-	6.50
	03-08	D1-62	108	66	ROUNDABOUT DESTINATION	-	2	-	-	49.50	-
	03-09	R5-1	30	30	DO NOT ENTER	-	1	-	-	6.25	-
	03-10	W14-3	48	36	NO PASSING ZONE	-	1	-	-	5.56	-
	03-11	W6-3	36	36	TWO-WAY TRAFFIC	-	1	-	-	9.00	-
	03-12	W14-3	48	36	NO PASSING ZONE	-	-	-	-	5.56	-
	04-01	J1-1	24	39	JCT	-	1	-	-	6.50	-
	04-02	W13-1	18	18	ADVISORY SPEED PLATE 20MPH	-	1	-	-	-	2.25
	04-03	W2-6	30	30	CIRCULAR INTERSECTION SIGN	-	-	-	-	-	6.25
	04-04	D1-62	144	66	ROUNDABOUT DESTINATION	-	2	-	-	66.00	-
	04-05	R2-1	24	30	SPEED LIMIT (55 MPH)	-	1	-	-	5.00	-
	04-06	W5-54	18	18	CLEARANCE MARKER	-	1	-	-	-	2.25
	04-07	R4-7	24	30	KEEP RIGHT	-	-	-	-	5.00	-
	04-08	J4-1	24	36	REASSURANCE ASSEMBLY	-	1	-	-	6.00	-
	04-09	R12-53	24	30	WEIGHT LIMIT CLASS B	-	1	-	-	5.00	-
	04-10	R5-1	30	30	DO NOT ENTER	-	1	-	-	6.25	-
	04-11	W6-3	36	36	TWO-WAY TRAFFIC	-	1	-	-	9.00	-
	05-01	R4-7	24	30	KEEP RIGHT	1	-	-	-	5.00	-
	05-02	R4-7	24	30	KEEP RIGHT	1	-	-	-	5.00	-
	05-03	W5-54	18	18	CLEARANCE MARKER	-	1	-	-	-	2.25
	05-04	R4-7	24	30	KEEP RIGHT	-	-	-	-	5.00	-
	05-05	J4-1	24	36	REASSURANCE ASSEMBLY	-	1	-	-	6.00	-
	05-06	D1-62	144	66	ROUNDABOUT DESTINATION	-	2	-	-	66.00	-
	05-07	W13-1	18	18	ADVISORY SPEED PLATE 20MPH	-	1	-	-	-	2.25
	05-08	W2-6	30	30	CIRCULAR INTERSECTION SIGN	-	1	-	-	-	6.25
	05-09	J1-1	24	39	JCT	-	1	-	-	6.50	-
	05-10	R5-1	30	30	DO NOT ENTER	-	1	-	-	6.25	-
	05-11	R2-1	24	30	SPEED LIMIT (55 MPH)	-	1	-	-	5.00	-
	05-12	W6-3	36	36	TWO-WAY TRAFFIC	-	1	-	-	9.00	-
					PAGE 2/2 - SUBTOTALS	2	30	-	-	314.37	38.75
					TOTAL 0010	2	40	10	12	555.47	49.50

ALL QUANTITIES CATEGORY 0010 UNLESS OTHERWISE NOTED

TRAFFIC CONTROL ITEMS

CATEGORY	LOCATION	APPROXIMATE SERVICE DAYS	NO.	643.0300 TRAFFIC CONTROL DRUMS DAY	NO.	643.0420 TRAFFIC CONTROL BARRICADES TYPE III DAY	NO.	643.0705 TRAFFIC CONTROL WARNING LIGHTS TYPE A DAY	NO.	643.0900 TRAFFIC CONTROL SIGNS DAY	NO.	CYCLES	643.0920 TRAFFIC CONTROL COVERING SIGNS TYPE II EACH	NO.	643.1050 TRAFFIC CONTROL SIGNS PCMS DAY
0010	ADVANCED WARNING	90	-	-	34	3,060	52	4,680	25	2,250	-	-	-	-	-
	DETOUR ROUTE PRE-WARN	7	10	70	-	-	-	-	-	-	-	-	-	4	28
	DETOUR ROUTE	90	-	-	-	-	-	-	168	15,120	13	1	13	-	-
	CTH J, NORTH LEG, DRIVEWAYS	90	25	2,250	-	-	-	-	-	-	-	-	-	-	-
	TOTAL 0010			2,320		3,060		4,680		17,370			13		28

PERMANENT MARKING ITEMS

STATION	TO	STATION	LOCATION	646.1020 MARKING LINE EPOXY 4-INCH (SOLID YELLOW) LF	646.1020 MARKING LINE EPOXY 4-INCH (SOLID WHITE) LF	646.1020 MARKING LINE EPOXY 4-INCH (SKIP WHITE) LF	646.1020 MARKING LINE EPOXY 4-INCH (SKIP YELLOW) LF	646.2020 MARKING LINE EPOXY 6-INCH (SOLID YELLOW) LF	646.2020 MARKING LINE EPOXY 6-INCH (SKIP WHITE)	646.2040 MARKING LINE GROOVED WET REF EPOXY 6-INCH (WHITE) LF	646.4020 MARKING LINE EPOXY 10-INCH (WHITE) LF	646.6320 MARKING DOTTED EXTENSION EPOXY 18-INCH (WHITE) LF	646.7120 MARKING DIAGONAL EPOXY 12-INCH (YELLOW) LF	646.7420 MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE) LF	646.8120 MARKING CURB EPOXY (YELLOW) LF	646.8220 MARKING ISLAND NOSE EPOXY (YELLOW) EACH
205+11 'EB'	-	210+78 'EB'	STH 11, WEST LEG	-	-	-	-	1,313	76	1,092	-	-	88	77	10	1
212+26 'EB'	-	217+76 'EB'	STH 11, EAST LEG	-	-	-	-	1,417	85	944	-	-	42	79	10	1
147+00 'JN'	-	151+37 'JN'	CTH J, SOUTH LEG	1286	728	78	-	-	-	-	-	-	53	69	10	1
152+85 'JN'	-	156+86 'JN'	CTH J, NORTH LEG	1116	648	91	228	-	-	-	-	-	50	72	30	3
10+00 'C'	-	13+46 'C'	ROUNDABOUT	38	-	-	-	35	-	-	243	142	-	-	-	-
			ITEM SUBTOTALS	2440	1376	169	228	2765	161	2036	243	142	233	297	60	6
			TOTAL 0010		4,213			2,926		2,036	243	142	233	297	60	6

CONSTRUCTION STAKING ITEMS

CATEGORY	STATION	TO	STATION	LOCATION	650.4000 CONSTRUCTION STAKING STORM SEWER EACH	650.4500 CONSTRUCTION STAKING SUBGRADE LF	650.5000 CONSTRUCTION STAKING BASE LF	650.5500 CONSTRUCTION STAKING CURB GUTTER AND CURB & GUTTER LF	650.6000 CONSTRUCTION STAKING PIPE CULVERTS EACH	650.7000 CONSTRUCTION STAKING CONCRETE PAVEMENT LF	650.9000 CONSTRUCTION STAKING CURB RAMPS EACH	650.9500.01 CONSTRUCTION STAKING SIDEWALK (PROJECT) (01.1320-07-73) EACH	650.9911.01 CONSTRUCTION STAKING SUPPLEMENTAL CONTROL (PROJECT) (01.1320-07-73) EACH	650.9920 CONSTRUCTION STAKING SLOPE STAKES LF
0010	205+11 'EB'	-	210+78 'EB'	STH 11, WEST LEG	3	1,135	1,135	874	1	60	4	-	-	1,135
	212+26 'EB'	-	217+76 'EB'	STH 11, EAST LEG	3	1,097	1,097	1,003	1	60	4	-	-	1,097
	147+00 'JN'	-	151+37 'JN'	CTH J, SOUTH LEG	3	879	879	716	2	-	4	-	-	879
	152+85 'JN'	-	156+86 'JN'	CTH J, NORTH LEG	3	798	798	497	2	-	4	-	-	798
	10+00 'C'	-	13+46 'C'	ROUNDABOUT	6	346	346	563	-	346	-	-	-	-
				PROJECT WIDE	-	-	-	-	-	-	-	1	1	-
				TOTAL 0010	18	4,255	4,255	3,653	6	466	16	1	1	3,909

ALL QUANTITIES CATEGORY 0010 UNLESS OTHERWISE NOTED

LIGHTING - CONDUIT AND WIRE

652.0225 CONDUIT RIGID NONMETALLIC SCHEDULE 40 2-INCH
652.0615 CONDUIT SPECIAL 3-INCH
655.0615 ELECTRICAL WIRE LIGHTING 10 AWG
655.0620 ELECTRICAL WIRE LIGHTING 8 AWG

CATEGORY	SYSTEM	CIRCUIT	FROM	TO	652.0225 CONDUIT RIGID NONMETALLIC SCHEDULE 40 2-INCH	652.0615 CONDUIT SPECIAL 3-INCH	655.0615 ELECTRICAL WIRE LIGHTING 10 AWG	655.0620 ELECTRICAL WIRE LIGHTING 8 AWG
					LF	LF	LF	LF
0010	RACINE COUNTY	A/N/B/GND	HL-51-CJ	LPB-CJ-1	10	--	--	120
			LPB-CJ-1	ACJ1	75	--	--	300
			ACJ1	BCJ1	185	--	555	--
			BCJ1	STUBOUT	15	--	45	--
			LPB-CJ-1	BCJ2	75	--	--	300
			BCJ2	ACJ2	120	--	--	480
			ACJ2	LPB-CJ-2	85	--	--	340
			LPB-CJ-2	LPB-CJ-3	--	85	--	340
			LPB-CJ-3	ACJ3	110	--	330	--
			ACJ3	STUBOUT	15	--	45	--
			LPB-CJ-3	BCJ3	50	--	--	200
			BCJ3	ACJ4	120	--	360	--
			ACJ4	LPB-CJ-4	95	--	--	--
			LPB-CJ-4	LPB-CJ-5	--	75	--	--
		C/N/D/GND	LPB-CJ-1	LPB-CJ-8	--	70	--	280
			LPB-CJ-8	CCJ1	115	--	--	460
			CCJ1	DCJ1	100	--	--	400
			DCJ1	LPB-CJ-7	45	--	--	180
			LPB-CJ-7	CCJ2	120	--	360	--
			CCJ2	STUBOUT	15	--	45	--
			LPB-CJ-7	LPB-CJ-6	--	70	--	280
			LPB-CJ-6	DCJ2	90	--	--	360
			DCJ2	CCJ3	115	--	--	460
			CCJ3	LPB-CJ-5	45	--	--	180
			LPB-CJ-5	DCJ3	110	--	--	440
			DCJ3	CCJ4	160	--	480	--
			CCJ4	STUBOUT	15	--	--	45
0010				TOTALS	1,350	230	1,335	3,220

LIGHTING - PULL BOXES

653.0140 PULL BOXES STEEL 24X42-INCH

CATEGORY	SYSTEM	DESCRIPTION	LOCATION	OFFSET	653.0140 PULL BOXES STEEL 24X42-INCH
					EACH
0010	RACINE COUNTY	LPB-CJ-1	209+54	24' RT	1
		LPB-CJ-2	212+41	17' RT	1
		LPB-CJ-3	210+59	16' RT	1
		LPB-CJ-4	213+42	12' RT	1
		LPB-CJ-5	213+42	7' LT	1
		LPB-CJ-6	210+68	9' LT	1
		LPB-CJ-7	153+99	27' LT	1
		LPB-CJ-8	209+49	41' LT	1
0010				TOTALS	8

ALL ITEMS CATEGORY 0010

LIGHTING - LIGHTS

654.0105 CONCRETE BASES TYPE 5
655.0610 ELECTRICAL WIRE LIGHTING 12 AWG
657.0255 TRANSFORMER BASES BREAKAWAY 11 1/2-INCH BOLT CIRCLE
657.0322 POLES TYPE 5 ALUMINUM
657.0610 LUMINAIRE ARMS SINGLE MEMBER 4 1/2-INCH CLAMP 6-FT
659.1115 LUMINAIRES UTILITY LED A

CATEGORY	SYSTEM	DESCRIPTION	LOCATION	OFFSET	654.0105 CONCRETE BASES TYPE 5	655.0610 ELECTRICAL WIRE LIGHTING 12 AWG	657.0255 TRANSFORMER BASES BREAKAWAY 11 1/2-INCH BOLT CIRCLE	657.0322 POLES TYPE 5 ALUMINUM	657.0610 LUMINAIRE ARMS SINGLE MEMBER 4 1/2-INCH CLAMP 6-FT	659.1115 LUMINAIRES UTILITY LED A
					EACH	LF	EACH	EACH	EACH	EACH
0010	RACINE COUNTY	BCJ1	206+76	24' RT	1	50	1	1	1	1
		ACJ1	208+62	24' RT	1	50	1	1	1	1
		BCJ2	210+07	33' RT	1	50	1	1	1	1
		ACJ2	211+50	18' RT	1	50	1	1	1	1
		BCJ3	211+05	19' RT	1	50	1	1	1	1
		ACJ3	149+23	25' RT	1	50	1	1	1	1
		ACJ4	212+47	19' RT	1	50	1	1	1	1
		CCJ1	210+63	18' LT	1	50	1	1	1	1
		DCJ1	211+91	16' LT	1	50	1	1	1	1
		CCJ2	155+17	24' LT	1	50	1	1	1	1
		DCJ2	211+62	19' LT	1	50	1	1	1	1
		CCJ3	213+02	16' LT	1	50	1	1	1	1
		DCJ3	214+51	23' LT	1	50	1	1	1	1
		CCJ4	216+07	24' LT	1	50	1	1	1	1
0010				TOTALS	14	700	14	14	14	14

ALL ITEMS CATEGORY 0010

LIGHTING - CONT, SERV AND SYST

654.0230	CONCRETE CONTROL CABINET BASES TYPE L30
655.0640	ELECTRICAL WIRE LIGHTING 1 AWG
656.0201	ELECTRICAL SERVICE MAIN LUGS ONLY METER PEDESTAL (HL-51-CJ)
659.2130	LIGHTING CONTROL CABINETS 120/240 30-INCH
SPV.0060.01	LIGHTING SYSTEM INTEGRATOR (1320-07-73)
SPV.0060.02	LIGHTING SYSTEM SURVEY (1320-07-73)

CATEGORY	STAGE	SYSTEM	DESCRIPTION	STATION	OFFSET	654.0230 CONCRETE CONTROL CABINET BASES TYPE L30	655.0640 ELECTRICAL WIRE LIGHTING 1 AWG	656.0201 ELECTRICAL SERVICE METER BREAKER PEDESTAL (HL-51-CJ)	659.2130 LIGHTING CONTROL CABINETS 120/240 30-INCH	SPV.0060.01 LIGHTING SYSTEM INTEGRATOR (1320-07-73)	SPV.0060.02 LIGHTING SYSTEM SURVEY (1320-07-73)
						EACH	LF	EACH	EACH	EACH	EACH
0010		RACINE COUNTY	HL-51-CJ	209+53	32' RT	1	18	--	1	--	--
		PROJECT				--	--	1	--	1	1
0010					TOTALS	1	18	1	1	1	1

ALL ITEMS CATEGORY 0010

CONVENTIONAL SYMBOLS

SECTION LINE
QUARTER LINE
SIXTEENTH LINE
NEW REFERENCE LINE
NEW R/W LINE
EXISTING R/W LINE
PROPERTY LINE
LOT, TIE, AND OTHER MINOR LINES
SLOPE INTERCEPT
CORPORATE LIMITS
UNDERGROUND FACILITY (COMMUNICATIONS, ELECTRIC, ETC)
FEE ACQUISITION AREA (HATCHING VARIES BY OWNER)
TEMP. LIMITED EASEMENT AREA
EASEMENT AREA (HIGHWAY, PERMANENT LIMITED, OR RESTRICTED DEVELOPMENT)
TRANSMISSION STRUCTURES
BUILDING
BUILDING (TO BE REMOVED)
BRIDGE

PARCEL NUMBER 25
UTILITY NUMBER 40
SECTION CORNER
NOTATION FOR COMBUSTABLE FLUIDS
NOTATION FOR HIGH VOLTAGE TRANSMISSION LINES
R/W MONUMENT
NON-MONUMENTED R/W POINT
FOUND IRON PIN IP
VALVE (GAS, WATER, ETC.) (TYPE)
SIGN
OFF-PREMISE SIGN #1-25

ACCESS CONTROLLED BY ACQUISITION
NO ACCESS (BY STATUTORY AUTHORITY)
ACCESS RESTRICTED (BY PREVIOUS PROJECT OR CONTROL)
NO ACCESS (NEW HIGHWAY)
NATIONAL GEODETIC SURVEY MONUMENT
SIXTEENTH CORNER MONUMENT
PARALLEL OFFSETS

CONVENTIONAL UTILITY SYMBOLS

	NON-COMPENSABLE	COMPENSABLE
WATER	W	
GAS	G	
TELEPHONE	T	
OVERHEAD TRANSMISSION LINES	OH	
ELECTRIC	E	
CABLE TELEVISION	TV	
FIBER OPTIC	FO	
SANITARY SEWER	SAN	
STORM SEWER	SS	
ELECTRIC TOWER		
POWER POLE		
TELEPHONE POLE		
TELEPHONE PEDESTAL		

CURVE DATA ABBREVIATIONS

LONG CHORD	LCH
LONG CHORD BEARING	LCB
RADIUS	R
DEGREE OF CURVE	D
CENTRAL ANGLE	Δ/DELTA
LENGTH OF CURVE	L
TANGENT	T
DIRECTION AHEAD	DA
DIRECTION BACK	DB

CONVENTIONAL ABBREVIATIONS

ACCESS RIGHTS	AR	OUTLOT	OL
ACRES	AC	PAGE	P
AHEAD	AH	POINT OF TANGENCY	PT
ALUMINUM	ALUM	PROPERTY LINE	PL
AND OTHERS	ET AL	RECORDED AS	(100')
BACK	BK	REEL / IMAGE	R/I
BLOCK	BLK	REFERENCE LINE	R/L
CENTERLINE	C/L	PERMANENT LIMITED EASEMENT	PLE
CERTIFIED SURVEY MAP	CSM	POINT OF BEGINNING	POB
CONCRETE	CONC	POINT OF CURVATURE	PC
COUNTY	CO	POINT OF COMPOUND CURVE	PCC
COUNTY TRUNK HIGHWAY	CTH	POINT OF INTERSECTION	PI
DISTANCE	DIST	REMAINING	REM
CORNER	COR	RESTRICTIVE DEVELOPMENT EASEMENT	RDE
DOCUMENT NUMBER	DOC	RIGHT	RT
EASEMENT	EASE	RIGHT OF WAY	R/W
EXISTING	EX	SECTION	SEC
GAS VALVE	GV	SEPTIC VENT	SEPV
GRID NORTH	GN	SQUARE FEET	SF
HIGHWAY EASEMENT	HE	STATE TRUNK HIGHWAY	STH
IDENTIFICATION	ID	STATION	STA
LAND CONTRACT	LC	TELEPHONE PEDESTAL	TP
LEFT	LT	TEMPORARY LIMITED EASEMENT	TLE
MONUMENT	MON	TRANSPORTATION PROJECT PLAT	TPP
NATIONAL GEODETIC SURVEY	NGS	UNITED STATES HIGHWAY	USH
NUMBER	NO	VOLUME	V

NOTES:

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COUNTY COORDINATES, RACINE COUNTY, NAD83(2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

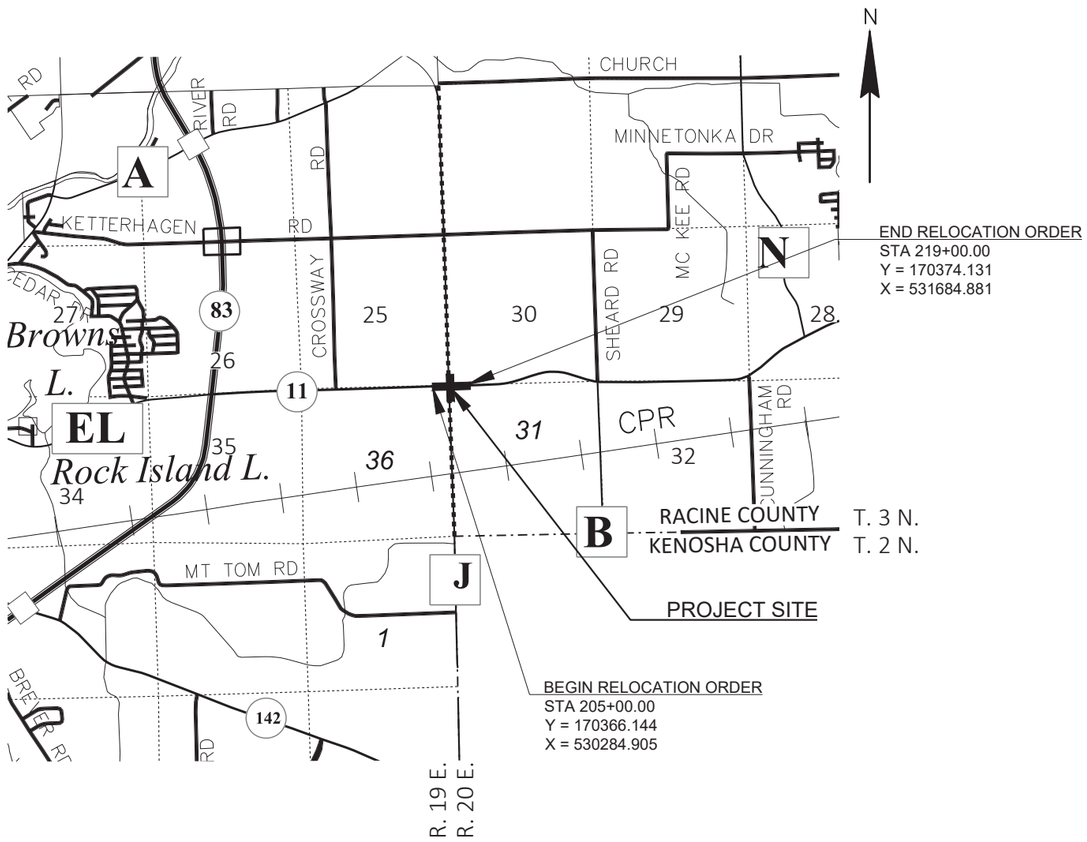
ALL RIGHT-OF-WAY LINES DEPICTED IN THE NON-ACQUISITION AREAS ARE INTENDED TO RE-ESTABLISH EXISTING RIGHT-OF-WAY LINES AS DETERMINED FROM PREVIOUS PROJECTS, OTHER RECORDED DOCUMENTS, OR FROM CENTERLINE OF EXISTING PAVEMENTS.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS" OF PUBLIC RECORD.

DIMENSIONING FOR THE NEW RIGHT-OF-WAY IS MEASURED ALONG AND PERPENDICULAR TO THE NEW REFERENCE LINES.

A TEMPORARY LIMITED EASEMENT (TLE) IS A RIGHT FOR CONSTRUCTION PURPOSES, AS DEFINED HEREIN, INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON, THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM DESIRABLE. ALL (TLEs) ON THIS PLAT EXPIRE AT THE COMPLETION OF THE CONSTRUCTION PROJECT FOR WHICH THIS INSTRUMENT IS GIVEN.

PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM DATA DERIVED FROM MAPS AND DOCUMENTS OF PUBLIC RECORD AND/OR EXISTING OCCUPATIONAL LINES. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.



LAYOUT
SCALE 0 1 MILE

TOTAL NET LENGTH OF CENTERLINE = 1,400'

CAUTION:
THIS PLAT IS FOR ILLUSTRATIVE PURPOSES ONLY. DEEDS MUST BE CHECKED TO DETERMINE PROPERTY BOUNDARIES.

R/W PROJECT NUMBER	SHEET NUMBER	TOTAL SHEETS
1320-07-23		
FEDERAL ID NUMBER	4.01	

PLAT OF RIGHT OF WAY REQUIRED FOR

DURAND AVE
STH 11 AND CTH J

STH 11 RACINE COUNTY

CONSTRUCTION NUMBER 1320-07-73

REVISION DATE:
04/19/2023

APPROVED
FOR
THE DEPARTMENT OF TRANSPORTATION

DATE

PLAT PREPARED BY
AYRES ASSOCIATES

THE SURVEY IS PREPARED AT THE REQUEST OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION.

THE FIELD SURVEY WAS PERFORMED IN DECEMBER 2018.

THIS SURVEY IS ACCURATE TO THE BEST OF MY KNOWLEDGE AND BELIEF.

WISCONSIN BRIAN F. GLASZCZ S-3118 WEST BEND WIS. LAND SURVEYOR

4/19/23

BRIAN GLASZCZ, P.L.S. DATE
S-3118

SCHEDULE OF LANDS & INTERESTS REQUIRED		OWNERS NAMES ARE SHOWN FOR REFERENCE PURPOSE ONLY AND ARE SUBJECT TO CHANGE PRIOR TO TRANSFER OF LAND AND INTERESTS TO THE DEPARTMENT					
PARCEL NUMBER	OWNER (S)	INTERESTS REQUIRED	R/W SQ FT REQUIRED			PLE SQ FT	TLE SQ FT
			NEW	EXISTING	TOTAL		
1	BRIAN J TORGERSON						1,297
2	IRREVOCABLE HENNINGFIELD FARM TRUST		5,753	63 618	69 371		
3	R&C WEIS FARMS LLC		30 441	133,140	163 581		

UTILITY INTERESTS REQUIRED		
PARCEL NUMBER	OWNER (S)	INTEREST REQUIRED
100	TDS TELECOM	RELEASE OF RIGHTS
101	ELIMINATED	
102	WE ENERGIES ELECTRIC	RELEASE OF RIGHTS
103	WE ENERGIES GAS	RELEASE OF RIGHTS

Point	Station	Offset
T150	209+69.13	-50.311'
T151	209+79.08	-50.290'
T152	209+87.35	-43.273'
T153	210+42.52	-43.144'
T154	210+84.62	-63.143'

HWY	BASIS OF EXISTING R/W	R/W WIDTH	YEAR
STH 11	R/W PROJECT 1320-06-20	VARIES	2002
STH 11	R/W PROJECT 439-C	VARIES	1930
CTH J	R/W PROJECT 1320-06-20	100'	2002

Point	Station	Offset
100	211+83.79	-275.24'
101	212+08.08	-153.52'
102	212+48.08	-118.16'
103	213+90.76	-72.56'
104	214+61.35	-60.01'
105	217+66.32	-60.00'
106	217+66.32	-50.00'
107	217+66.32	0.00'
108	218+43.17	0.00'
109	218+43.17	50.00'
110	218+43.17	65.00''
111	216+86.90	65.00'
112	213+33.63	100.17'
113	212+45.00	148.59'
114	212+12.24	191.44'
115	211+87.98	472.25'
116	211+20.98	472.71'
117	211+20.81	448.66'
118	210+87.04	448.83'
119	210+66.17	406.70'
123	206+05.93	60.00'
124	206+05.93	0.000'
125	206+05.93	-1.07'
126	211+17.73	0.000'
127	211+17.73	-275.07'
129	212+14.95	290.04'

Course	Bearing	Distance
126-127	N00° 28' 41"W	275.08'
127-100	N89° 34' 48" E	67.00'
100-101	S11° 33' 58"E	124.11'
101-102	S48° 48' 29"E	53.40'
102-103	S72° 33' 40"E	149.79'
103-104	S80° 12' 06"E	71.70'
104-105	N89° 43' 01"E	304.97'
105-106	S00° 16' 59"E	10.00'
106-107	S00° 16' 59"E	50.00'
107-108	N89° 43' 01"E	76.86'
108-109	S00° 16' 59"E	50.00'
109-110	S00° 16' 59"E	15.00'
110-111	S89° 43' 01"W	156.27'
111-112	S84° 01' 52"W	355.03'
112-113	S61° 03' 47"W	100.99'
113-114	S37° 07' 12"W	53.94'
114-129	S01° 51' 31"E	98.64'
129-115	S08° 08' 19"W	184.19'
115-116	S89° 19' 23"W	67.00'
116-117	N00° 40' 37"W	74.05'
117-118	S89° 19' 23"W	33.00'
118-119	N26° 43' 48"W	47.02'
119-120	N00° 24' 40"W	200.00'
120-121	N21° 56' 55"W	78.10'
121-122	N72° 39' 54"W	153.70'
122-123	N84° 54' 47"W	286.33'
123-124	N00° 22' 57"W	60.00'
124-125	N00° 22' 57"W	1.07'
125-126	N89° 44' 15"E	511.80'

SE-SE

UNPLATTED LANDS

TLE = 1297 SQ FT
BRIAN J TORGERSON (1)

CONCRETE MON.
WITH BRASS CAP
Y = 173003.241
X = 530880.685

SECTION LINE S0° 28' 41"E 2633.06 MON. TO MON.

P.I.: 211+17.73

IRREVOCABLE HENNINGFIELD FARM TRUST (2)

SW-SW

UNPLATTED LANDS

CONCRETE MON.
WITH BRASS CAP
Y = 170382.720
X = 533434.066

STH 11

SECTION LINE S89° 44' 15"W 2619.38 MON. TO MON.

RL=N89°37'03"E

SECTION LINE S89° 43' 01"W 2531.46 MON. TO MON.

RL=N89°43'01"E

CONCRETE MON.
WITH BRASS CAP
Y = 170358.089
X = 528283.382

R&C WEIS FARMS LLC (3)

SECTION LINE S0° 40' 37"E 2623.77 MON. TO MON.

CONCRETE MON.
WITH BRASS CAP
Y = 170370.290
X = 530902.602

R&C WEIS FARMS LLC (3)

NE-NE

UNPLATTED LANDS

TOWN OF BURLINGTON

COUTY ROAD J

CONCRETE MON.
WITH BRASS CAP
Y = 167746.535
X = 530933.744

TOWN OF DOVER

NW-NW

UNPLATTED LANDS

(100) TDS TELECOM
VOL.2281, P.116, DOC.1429450 - PARCEL 3
VOL.2281, P.118, DOC.1429451 - PARCEL 3
CONVEYANCE OF RIGHTS DOC. 1958197

(102) WE ENERGIES ELECTRIC
NO RECORD OF EASEMENT (AERIAL) - PARCEL 2
VOL.333, P.523 DOC.425393 - PARCEL 3
CONVEYANCE OF RIGHTS DOC.1939506

(103) WE ENERGIES GAS
VOL.333, P.523 DOC.425393 - PARCEL 3
CONVEYANCE OF RIGHTS DOC.1957309

NOTES:

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COUNTY COORDINATES, RACINE COUNTY, NAD83(2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY ¾" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

FOR THE LATEST ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN WAUKESHA

All TLE'S ARE FOR SLOPES AND GRADING UNLESS OTHERWISE STATED IN THE DETAIL.

WISCONSIN BRIAN F. GLASZCZ S-3118 WEST BEND WIS. LAND SURVEYOR

REVISION DATE: 04/19/2023

DATE _____

SCALE, FEET 0 50 100

HWY: STH 11

COUNTY: RACINE

STATE R/W PROJECT NUMBER 1320-07-23

CONSTRUCTION PROJECT NUMBER 1320-07-73

PLAT SHEET 4.01

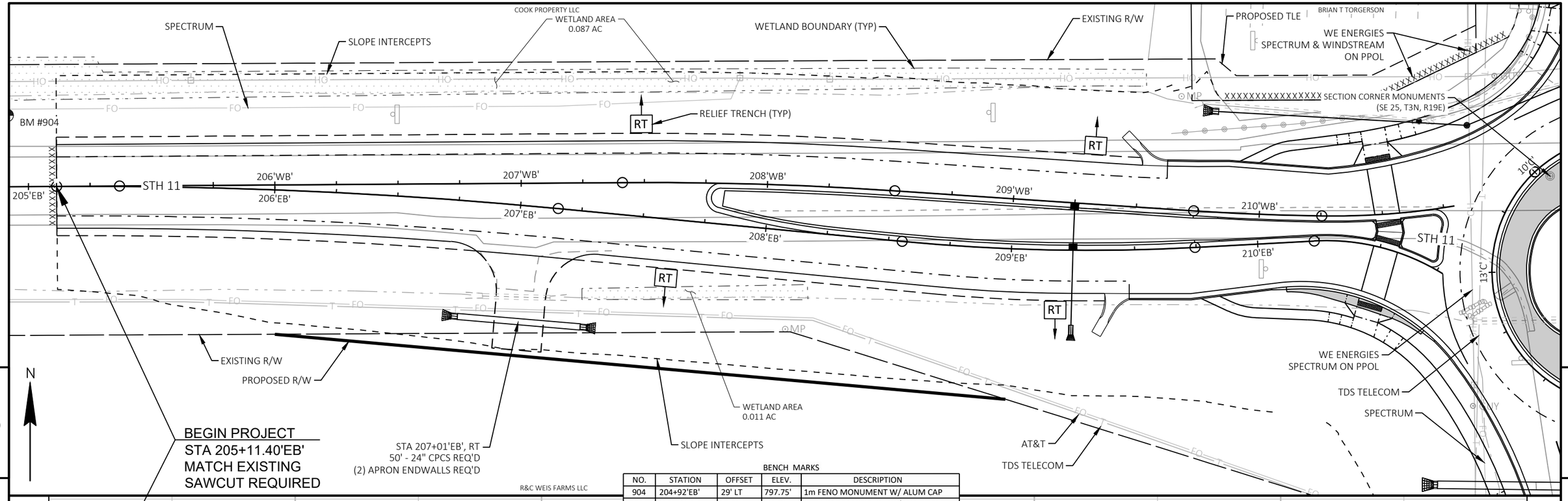
PS&E SHEET 80

E

FILE NAME : 4.01.DWG
LAYOUT NAME - TPP34 1 IN 100 FT

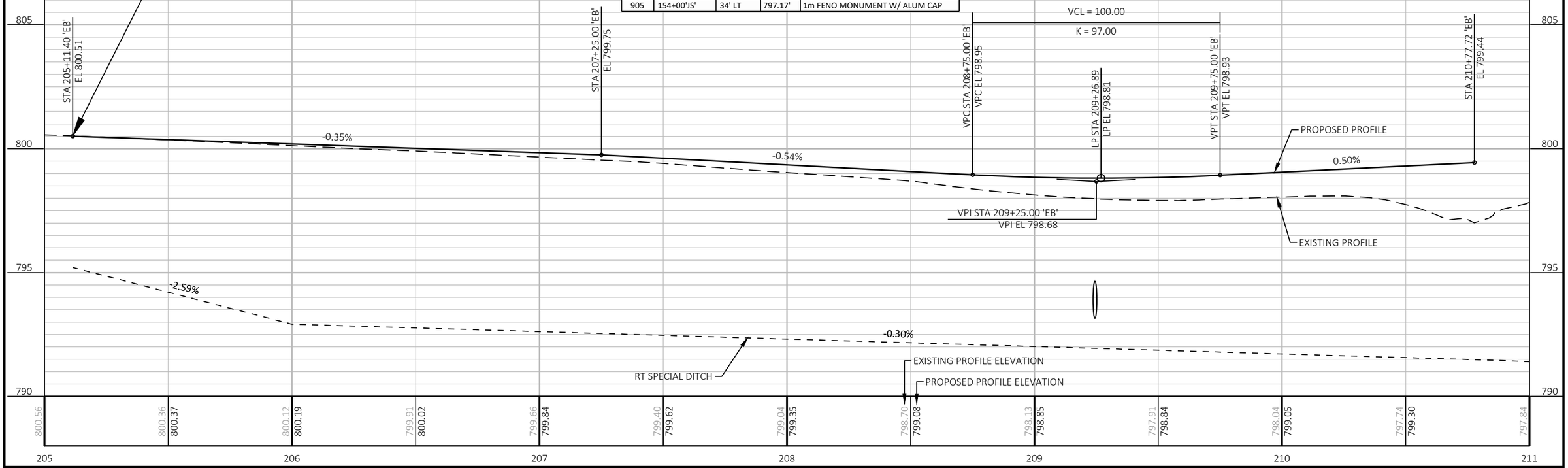
PLOT DATE : 4/19/2023 1:33 PM PLOT BY : LARSON, BENJAMIN PLOT NAME :

WISDOT/CADDS SHEET 75



BENCH MARKS

NO.	STATION	OFFSET	ELEV.	DESCRIPTION
904	204+92'EB'	29' LT	797.75'	1m FENO MONUMENT W/ ALUM CAP
905	154+00'JS'	34' LT	797.17'	1m FENO MONUMENT W/ ALUM CAP



PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	PLAN AND PROFILE: EB STH 11	SHEET 81	E

WE ENERGIES
SPECTRUM ON PPOL

PROPOSED R/W

SLOPE INTERCEPTS

DAVID HENNINGFIELD,
IRREVOCABLE HENNINGFIELD FARM TRUST

N

EXISTING R/W

RELIEF TRENCH (TYP)

RT

215'WB'

216'WB'

217'WB'

217'EB'

STH 11

218'EB'

216'EB'

214'WB'

213'WB'

215'EB'

214'EB'

213'EB'

11'C'

WETLAND AREA
0.026 AC

WETLAND
BOUNDARY
(TYP)

TDS TELECOM

EXISTING R/W

AT&T

70' - 36" CPCS TO BE REMOVED
(1) EXISTING APRON ENDWALL TO BE REMOVED

STA 217+79 'EB'
(1) APRON ENDWALL REQ'D
CONNECT TO EXISTING 36" CPCS

END PROJECT
STA 217+76.03'EB'
MATCH EXISTING
SAW CUT REQUIRED

PROPOSED R/W

SLOPE INTERCEPTS

R&C WEIS FARMS LLC

5

5

BENCH MARKS

NO.	STATION	OFFSET	ELEV.	DESCRIPTION
902	220+50'EB'	28' LT	793.33'	1m FENO MONUMENT W/ ALUM CAP
905	154+00'JS'	34' LT	797.17'	1m FENO MONUMENT W/ ALUM CAP

STA 212+26.05
EL 798.87

STA 215+85.00 'EB'
EL 797.08

STA 217+76.03 'EB'
EL 796.27

PROPOSED PROFILE

-0.50%

-0.42%

EXISTING PROFILE

RT SPECIAL DITCH

-0.30%

EXISTING PROFILE ELEVATION

PROPOSED PROFILE ELEVATION

800
795
790
788

792.89	793.05	798.75	792.63	798.50	793.67	798.25	794.85	798.00	795.82	797.75	796.14	797.50	796.23	797.25	796.16	797.02	796.14	796.80	796.24	796.59	796.30	796.38	796.23

212 213 214 215 216 217 218

PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	PLAN AND PROFILE: EB STH 11	SHEET 82	E

COOK PROPERTY LLC

BRIAN T TORGERSON

EXISTING R/W

SLOPE INTERCEPTS

WETLAND AREA
0.087 AC

WETLAND BOUNDARY (TYP)

PROPOSED TLE

WE ENERGIES
SPECTRUM & WINDSTREAM
ON PPOL

SECTION CORNER MONUMENTS
(SE 25, T3N, R19E)

SPECTRUM

RELIEF TRENCH (TYP)

RT

STH 11

206'WB'

206'EB'

207'WB'

207'EB'

208'WB'

208'EB'

209'WB'

209'EB'

210'WB'

210'EB'

10'C

13'C

N

EXISTING R/W

PROPOSED R/W

WETLAND AREA
0.011 AC

SLOPE INTERCEPTS

WE ENERGIES
SPECTRUM ON PPOL

TDS TELECOM

AT&T

TDS TELECOM

SPECTRUM

BEGIN CONSTRUCTION
STA 205+11.40'WB'
MATCH EXISTING
SAW CUT REQ'D

R&C WEIS FARMS LLC

BENCH MARKS

NO.	STATION	OFFSET	ELEV.	DESCRIPTION
904	204+92'EB'	29' LT	797.75'	1m FENO MONUMENT W/ ALUM CAP
905	154+00'JS'	34' LT	797.17'	1m FENO MONUMENT W/ ALUM CAP

STA 205+11.40 'WB'
EL 800.54

STA 207+25.00 'WB'
EL 799.51

VCL = 100.00
K = 100.06

VPC STA 208+75.00 'WB'
VPC EL 798.76

LP STA 209+24.97
LP EL 798.64

VPT STA 209+75.00 'WB'
VPT EL 798.76

STA 210+79.42 'WB'
EL 799.28

PROPOSED PROFILE

EXISTING PROFILE

-0.48%

-0.50%

0.50%

VPI STA 209+25.00 'WB'
VPI EL 798.51

-0.30%

LT SPECIAL DITCH

EXISTING PROFILE ELEVATION

PROPOSED PROFILE ELEVATION

805
800
795
790

800.36	800.35	800.16	800.11	799.97	799.87	799.78	799.63	799.55	799.39	799.30	799.14	799.05	798.89	798.86	798.67	798.59	798.67	798.40	798.89	798.24	799.14	798.15

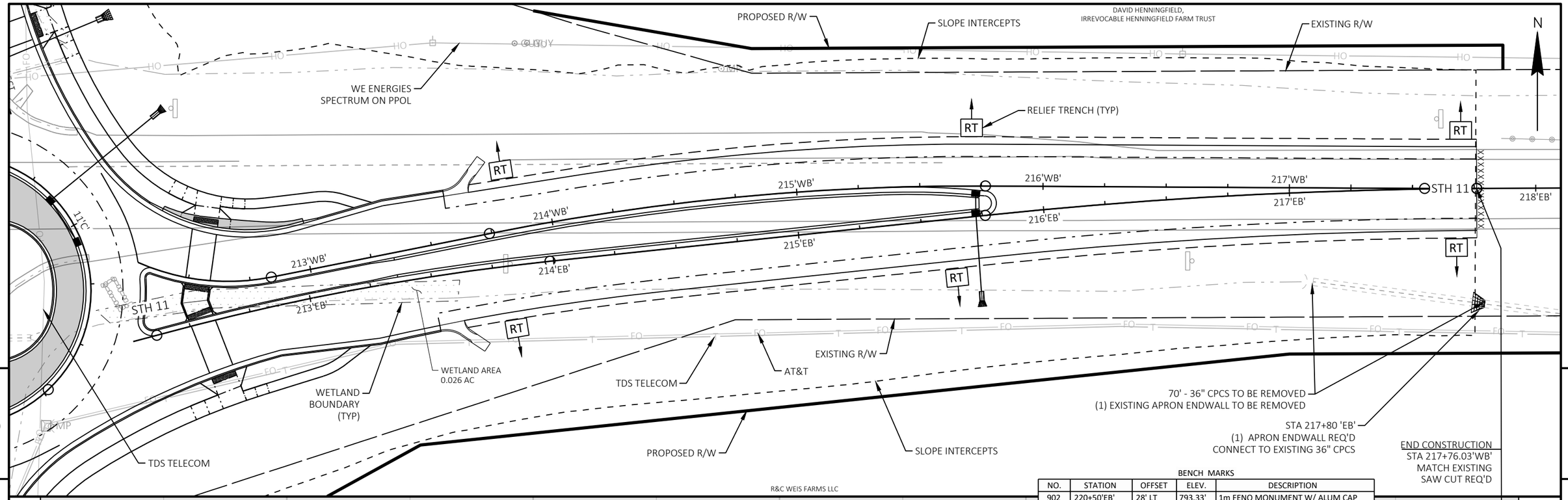
205 206 207 208 209 210 211

PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	PLAN AND PROFILE: WB STH 11	SHEET 83	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\050101-PP.DWG
LAYOUT NAME - WB_01

PLOT DATE : 1/19/2024 8:45 AM PLOT BY : MUENCH, DOUGLAS PLOT NAME : PLOT SCALE : 1 IN:40 FT

WISDOT/CADDS SHEET 44

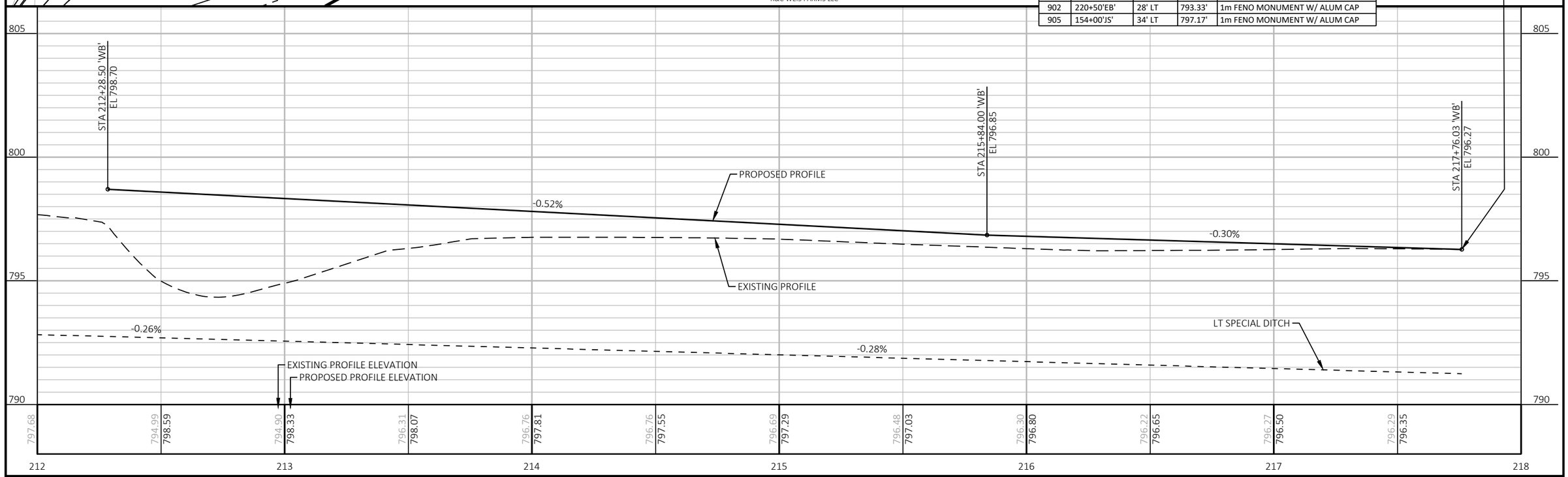


5

5

BENCH MARKS

NO.	STATION	OFFSET	ELEV.	DESCRIPTION
902	220+50'EB'	28' LT	793.33'	1m FENO MONUMENT W/ ALUM CAP
905	154+00'JS'	34' LT	797.17'	1m FENO MONUMENT W/ ALUM CAP



PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	PLAN AND PROFILE: WB STH 11	SHEET 84	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\050101-PP.DWG
LAYOUT NAME - WB_02

PLOT DATE : 1/19/2024 8:45 AM PLOT BY : MUENCH, DOUGLAS PLOT NAME : PLOT SCALE : 1 IN:40 FT

R&C WEIS FARMS LLC

N

SPECTRUM

EXISTING R/W

WE ENERGIES
SPECTRUM ON PPOL

SLOPE INTERCEPTS

TDS TELECOM

PROPOSED R/W

WETLAND BOUNDARY (TYP)

WETLAND AREA
0.017 AC

TEMPORARY CONSTRUCTION
EASEMENT

AT&T

STH 11

13'C'

10'C'

11'C'

12'C'

SECTION CORNER MONUMENTS
(SE 25, T3N, R19E)

CTH J

147'JS'
147'JN'

148'JS'
148'JN'

149'JS'
149'JN'

150'JS'
150'JN'

151'JS'
151'JN'

RT

RELIEF TRENCH (TYP)

STH 11

EXISTING R/W

BEGIN CONSTRUCTION
STA 146+99.65'JN'
MATCH EXISTING
SAW CUT REQ'D

STA 148+62'JN', RT
46' - 18" CPCS REQ'D
(2) APRON ENDWALLS REQ'D

SLOPE INTERCEPTS

PROPOSED R/W

TEMPORARY CONSTRUCTION
EASEMENT

12" CONCRETE CASING PIPE
(SEE STORM SEWER DETAIL SHEET)

TDS TELECOM

TDS TELECOM

BENCH MARKS

NO.	STATION	OFFSET	ELEV.	DESCRIPTION
903	145+97'JN'	26' RT	796.47'	1m FENO MONUMENT W/ ALUM CAP
905	154+00'JS'	34' LT	797.17'	1m FENO MONUMENT W/ ALUM CAP

R&C WEIS FARMS LLC

STA 146+99.65 'JN'
EL 798.27

STA 148+05.00 'JN'
EL 797.89

STA 149+30.00 'JN'
EL 798.27

STA 151+37.48 'JN'
EL 799.38

PROPOSED PROFILE

EXISTING PROFILE

-0.36%

0.30%

0.53%

-0.94%

0.73%

EXISTING PROFILE ELEVATION
PROPOSED PROFILE ELEVATION

RT SPECIAL DITCH

STA 150+38 'JN'
(1) - 176' CPRC CLASS III 12-INCH REQ'D
SKEW 5° LHF

STA 151+23 'JN'
(1) - 146' CPRC CLASS III 30-INCH REQ'D
(2) - APRON ENDWALLS REQ'D
SKEW 24° RHF

805
800
795
790

798.20	798.31	798.27	798.28	798.09	798.25	797.91	798.25	798.03	798.27	798.18	798.22	798.38	798.09	798.64	798.24	798.91	798.25	799.18	797.90	797.80	798.01

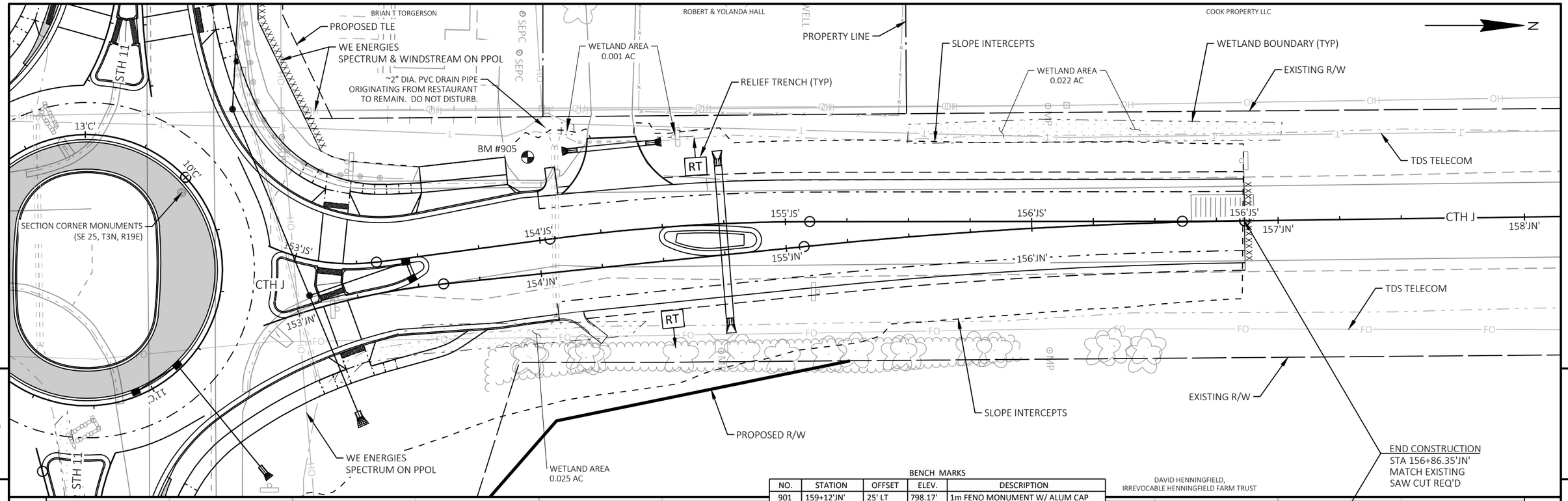
146 147 148 149 150 151 152 152

PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	PLAN AND PROFILE: NB CTH J - 'JN'	SHEET 85	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\050101-PP.DWG
LAYOUT NAME - NB_01

PLOT DATE : 1/19/2024 8:45 AM PLOT BY : MUENCH, DOUGLAS PLOT NAME : PLOT SCALE : 1 IN:40 FT

WISDOT/CADDS SHEET 44

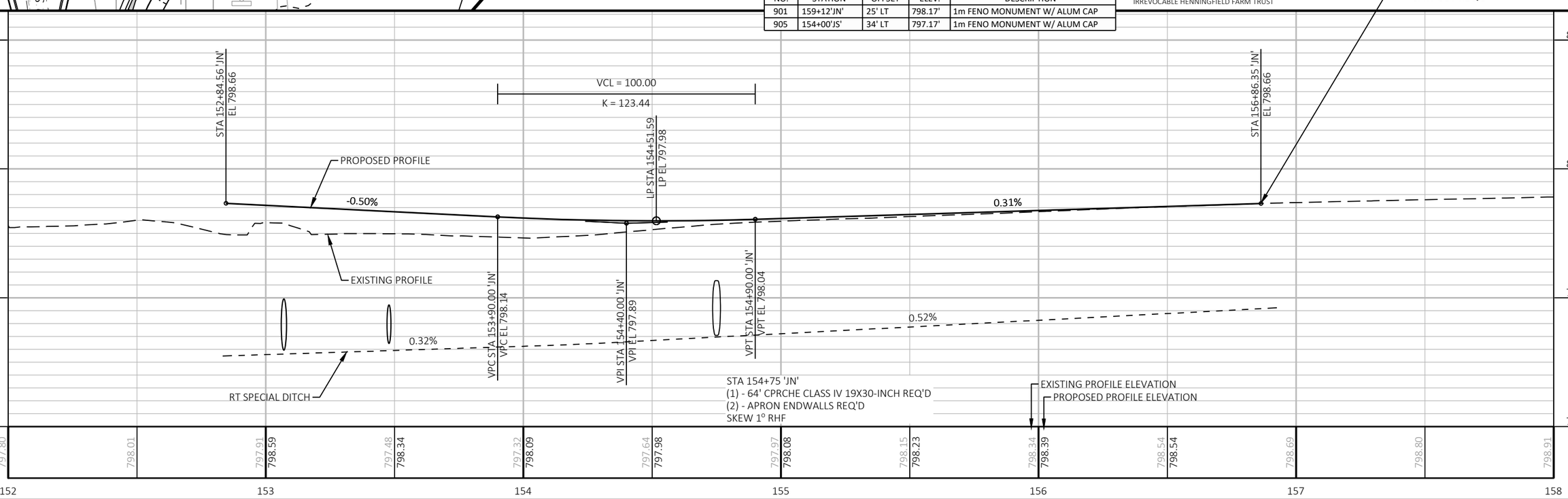


5

5

BENCH MARKS

NO.	STATION	OFFSET	ELEV.	DESCRIPTION
901	159+12'JN'	25' LT	798.17'	1m FENO MONUMENT W/ ALUM CAP
905	154+00'JS'	34' LT	797.17'	1m FENO MONUMENT W/ ALUM CAP



PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	PLAN AND PROFILE: NB CTH J - 'JN'	SHEET 86	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\050101-PP.DWG
LAYOUT NAME - NB_02
PLOT DATE : 1/19/2024 8:45 AM
PLOT BY : MUENCH, DOUGLAS
PLOT NAME :
PLOT SCALE : 1 IN:40 FT
WISDOT/CADDS SHEET 44

R&C WEIS FARMS LLC

TEMPORARY CONSTRUCTION EASEMENT

N

WE ENERGIES
SPECTRUM ON PPOL

SLOPE INTERCEPTS

PROPOSED R/W

WETLAND BOUNDARY (TYP)

WETLAND AREA
0.017 AC

TDS TELECOM

AT&T

SPECTRUM

EXISTING R/W

RELIEF TRENCH (TYP)

RT

13'C'

10'C'

SECTION CORNER MONUMENTS
(SE 25, T3N, R19E)

CTH J

147'JS'
147'JN'

148'JS'
148'JN'

149'JS'
149'JN'

150'JS'
150'JN'

151'JS'
151'JN'

12'C'

11'C'

5

EXISTING R/W

BEGIN CONSTRUCTION
STA 146+99.65'JS'
MATCH EXISTING
SAW CUT REQ'D

TDS TELECOM

12" CONCRETE CASING PIPE
SEE STORM SEWER DETAIL SHEET

SLOPE INTERCEPTS

PROPOSED R/W

TEMPORARY CONSTRUCTION EASEMENT

TDS TELECOM

BENCH MARKS

NO.	STATION	OFFSET	ELEV.	DESCRIPTION
903	145+97'JN'	26' RT	796.47'	1m FENO MONUMENT W/ ALUM CAP
905	154+00'JS'	34' LT	797.17'	1m FENO MONUMENT W/ ALUM CAP

R&C WEIS FARMS LLC

STA 146+99.65
EL 798.27

STA 148+05.00
EL 797.95

STA 149+30.00
EL 798.46

STA 151+42.43
EL 799.55

PROPOSED PROFILE

-0.30%

0.40%

0.51%

EXISTING PROFILE

-1.52%

-0.88%

LT SPECIAL DITCH

EXISTING PROFILE ELEVATION

PROPOSED PROFILE ELEVATION

STA 150+38 'JN'
(1) - 176' CPRC CLASS III 12-INCH REQ'D

STA 151+23 'JS'
(1) - 146' CPRC CLASS III 30-INCH REQ'D
(2) - APRON ENDWALLS REQ'D
SKEW 16° LHF

805
800
795
790

798.31	798.27	798.27	798.12	798.26	797.97	798.28	798.13	798.33	798.34	798.35	798.56	798.37	798.82	798.38	799.07	798.27	799.33	798.07	798.09	152+24.90	798.03	798.22

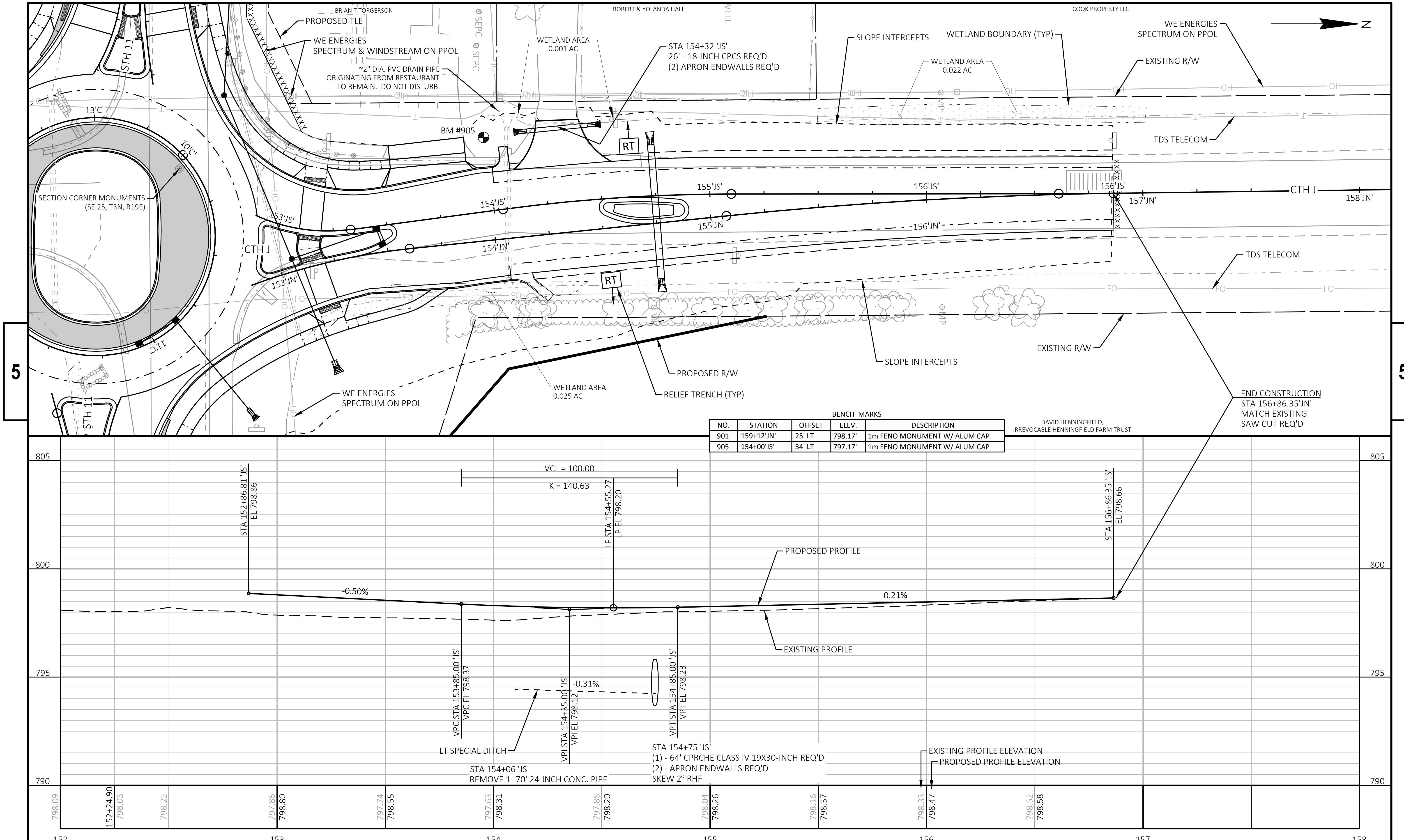
146 147 148 149 150 151 152 152

PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	PLAN AND PROFILE: SB CTH J - 'JS'	SHEET 87	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\050101-PP.DWG
LAYOUT NAME - SB_01

PLOT DATE : 1/19/2024 8:45 AM PLOT BY : MUENCH, DOUGLAS PLOT NAME : PLOT SCALE : 1 IN:40 FT

WISDOT/CADDS SHEET 44

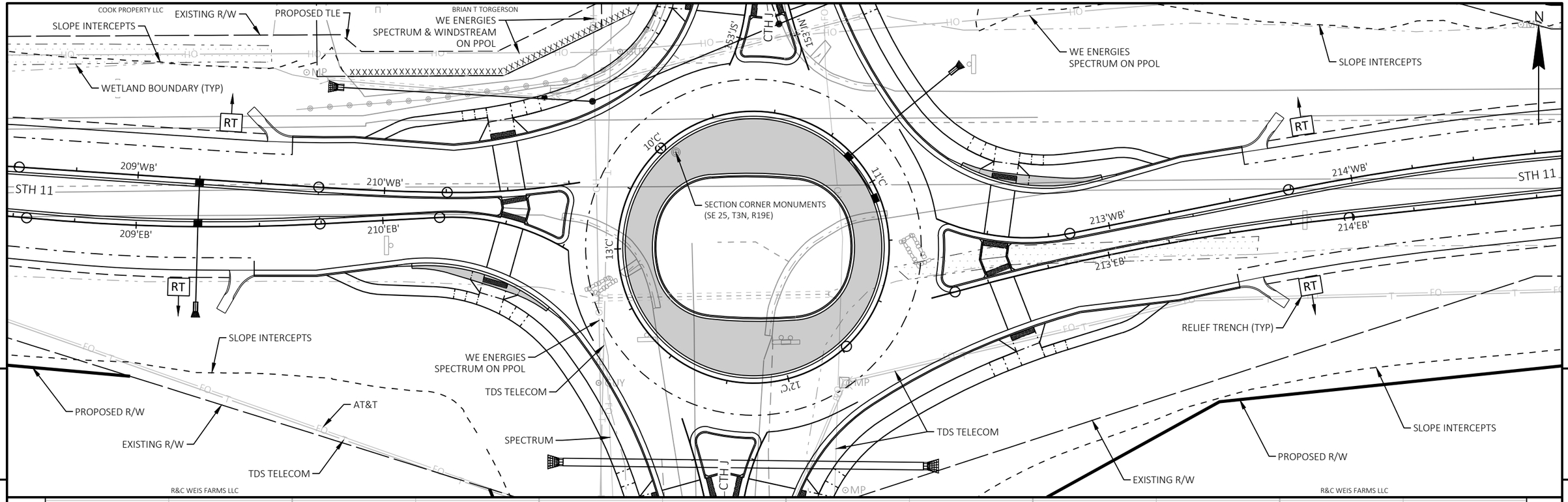


BENCH MARKS

NO.	STATION	OFFSET	ELEV.	DESCRIPTION
901	159+12'JN'	25' LT	798.17'	1m FENO MONUMENT W/ ALUM CAP
905	154+00'JS'	34' LT	797.17'	1m FENO MONUMENT W/ ALUM CAP

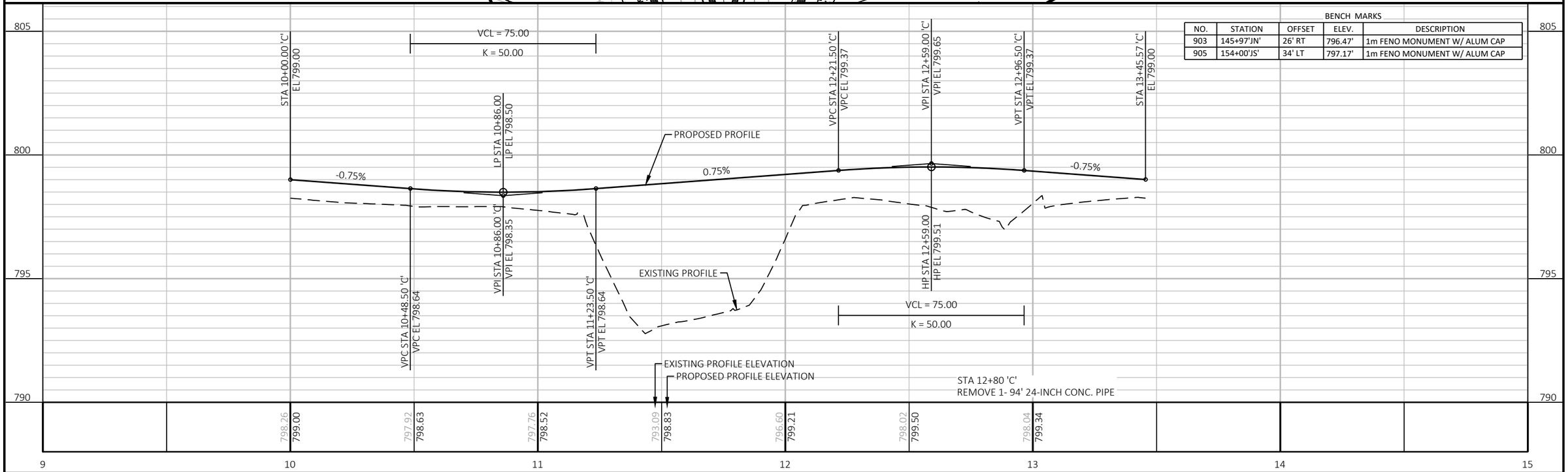
PROJECT NO: 1320-07-73 | HWY: STH 11 | COUNTY: RACINE | PLAN AND PROFILE: SB CTH J - 'JS' | SHEET 88 | E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\050101-PP.DWG
LAYOUT NAME - SB_02
PLOT DATE : 1/19/2024 8:45 AM PLOT BY : MUENCH, DOUGLAS PLOT NAME : PLOT SCALE : 1 IN:40 FT



BENCH MARKS

NO.	STATION	OFFSET	ELEV.	DESCRIPTION
903	145+97'JN'	26' RT	796.47'	1m FENO MONUMENT W/ ALUM CAP
905	154+00'JS'	34' LT	797.17'	1m FENO MONUMENT W/ ALUM CAP



PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	PLAN AND PROFILE: ROUNDABOUT	SHEET 89	E

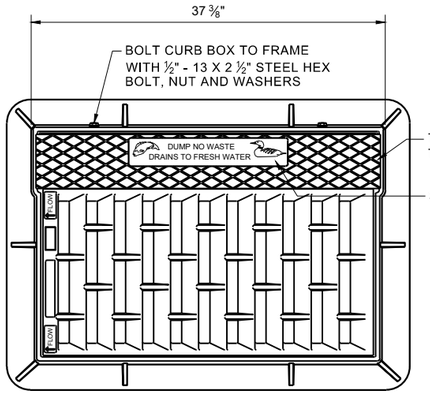
FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\050101-PP.DWG
LAYOUT NAME - C_01

PLOT DATE : 1/19/2024 8:45 AM PLOT BY : MUENCH, DOUGLAS PLOT NAME : PLOT SCALE : 1 IN:40 FT

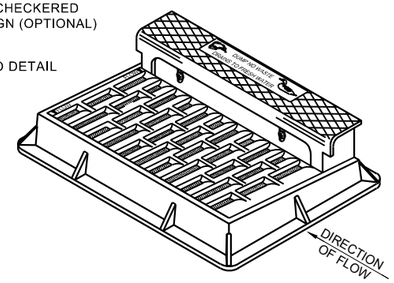
WISDOT/CADDS SHEET 44

Standard Detail Drawing List

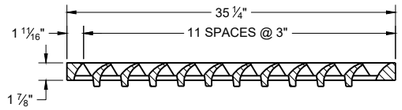
08A05-20A	INLET COVERS TYPE A, H, A-S, H-S & Z
08A05-20C	INLET COVERS TYPE F, HM, HM-S, S, T, V, HM-GJ, & HM-GJ-S
08A05-20D	INLET COVER TYPE BW, MANHOLE COVERS, TYPE K, J, J-S, L & M
08B09-03	MANHOLES 3-FT, 4-FT, 5-FT, 6-FT, 7-FT, 8-FT, 9-FT, 10-FT DIAMETER
08C07-02	INLETS 2X2-FT, 2X2.5-FT, 2X3-FT AND 2.5X3-FT
08D01-23A	CONCRETE CURB & GUTTER
08D01-23B	CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS
08D04-07	CONCRETE SURFACE DRAINS & ASPHALTIC FLUMES
08D05-21B	CURB RAMPS TYPES 2 AND 3
08D05-21E	CURB RAMPS TYPES 5, 6, 7A, 7B & 8
08D05-21G	CURB RAMPS RECTANGULAR AND RADIAL DETECTABLE WARNING PLATES
08E09-06	SILT FENCE
08E10-02	INLET PROTECTION TYPE A, B, C AND D
08E15-01	CULVERT PIPE CHECK
09B02-10	CONDUIT
09B04-12	PULL BOX
09C02-09	CONCRETE BASES, TYPES 1, 2, 5, & 6
09C03-04	TRANSFORMER/PEDESTAL BASES
09D04-03	LIGHTING CONTROL CABINET 120/240 VOLT
09E01-15D	POLE MOUNTINGS FOR LIGHTING UNITS, TYPE 5 (30 FEET)
09E01-15G	HARDWARE DETAILS FOR POLE MOUNTINGS
09E03-06	NON-FREEWAY LIGHTING UNIT POLE WIRING
11B02-02	CONCRETE MEDIAN NOSE
13C18-08A	CONCRETE PAVEMENT JOINTING
13C18-08B	CONCRETE PAVEMENT STEEL REINFORCEMENT
13C18-08C	CONCRETE PAVEMENT JOINT TYPES
13C18-08D	CONCRETE PAVEMENT JOINTING AT UTILITY FIXTURES
13C18-08E	CONCRETE PAVEMENT JOINTING AND STEEL REINFORCEMENT IN ROUNDABOUTS
13C18-08F	CONCRETE PAVEMENT INTERSECTION BOXOUT FOR INTEGRAL CURB AND GUTTER
13C19-03	HMA LONGITUDINAL JOINTS
15A01-13A	MARKER POST FOR RIGHT-OF-WAY
15A03-02A	FLEXIBLE MARKER POST FOR CULVERT END
15A03-02B	FLEXIBLE MARKER POST FOR CULVERT END
15C02-09A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-09B	BARRICADES AND SIGNS FOR VARIOUS CLOSURES
15C02-09C	DETOUR SIGNING FOR MAINLINE CLOSURES
15C04-05	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M.P.H. OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC
15C11-10B	CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS
15C18-08A	MEDIAN ISLAND MARKING PAVEMENT MARKINGS
15C18-08B	MEDIAN ISLAND MARKING MEDIAN ISLAND NOSE
15D51-01	TRAFFIC CONTROL, MOBILE OPERATIONS ON AN UNDIVIDED ROADWAY

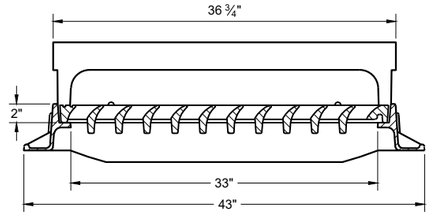


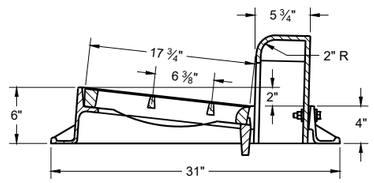
NOTE: EITHER CASTING IS ACCEPTABLE

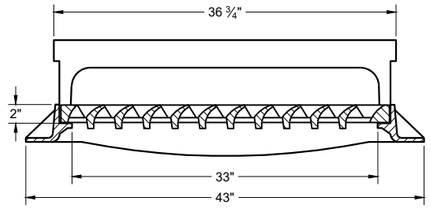


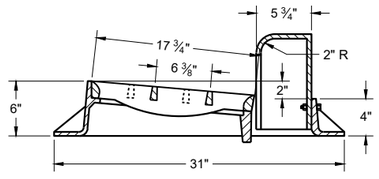
NOTE: CURB BOX HEIGHT ADJUSTABLE 6" - 9"







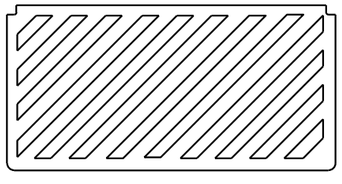




TYPE "H"

NOTE: EITHER CASTING IS ACCEPTABLE

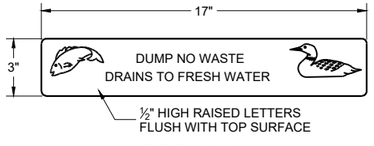
1 ⅛" DIAGONAL BARS WITH 1 ⅝" OPENINGS



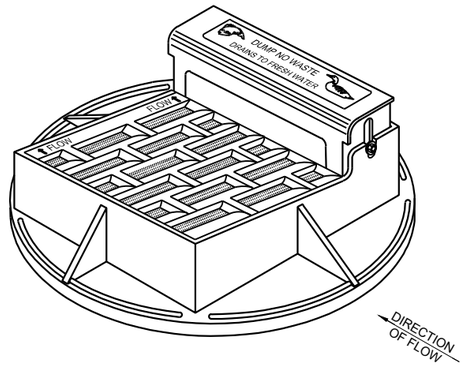
SPECIAL GRATE FOR TYPE "H" COVER

(MEASURES 35" X 17 ¾" X 2")

(NOTED AS TYPE H-S ON DRAINAGE TABLE)

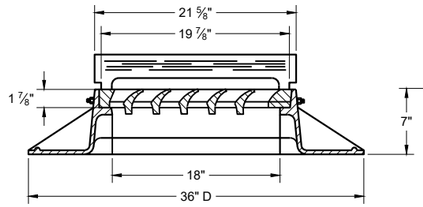


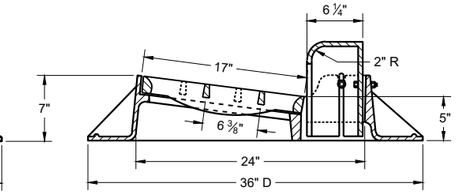
LOGO DETAIL



NOTE: CURB BOX HEIGHT ADJUSTABLE 6" - 9"

NOTE: EITHER CASTING IS ACCEPTABLE





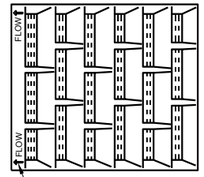
TYPE "A"

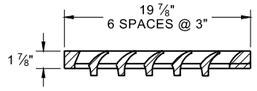
GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

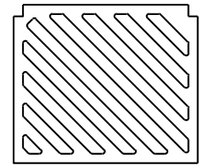
DETAIL DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR CATCH BASIN, MANHOLE AND INLET COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ROUND FRAMES AND COVERS SHALL HAVE CONTINUOUSLY MACHINED BEARING SURFACES TO PREVENT ROCKING AND RATTLING.





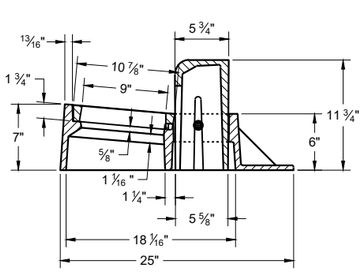
1" DIAGONAL BARS WITH 1 ½" OPENINGS

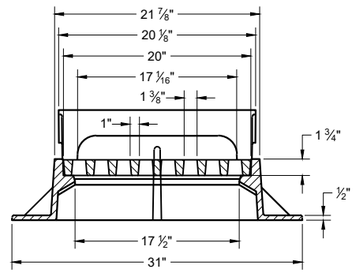


SPECIAL GRATE FOR TYPE "A" COVER

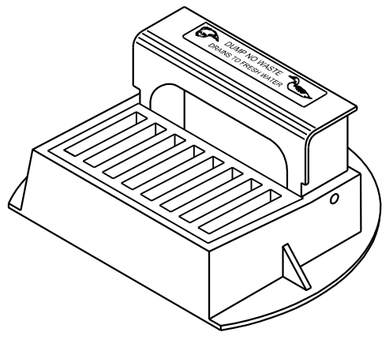
(MEASURES 19 ¾" X 17" X 1 ⅞")

(NOTED AS TYPE A-S ON DRAINAGE TABLE)





TYPE "Z"



INLET COVERS
TYPES A, H, A-S, H-S AND Z

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
July 2023
DATE

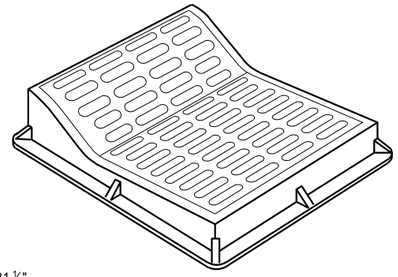
/S/ Rodney Taylor
ROADWAY STANDARDS DEVELOPMENT
UNIT SUPERVISOR

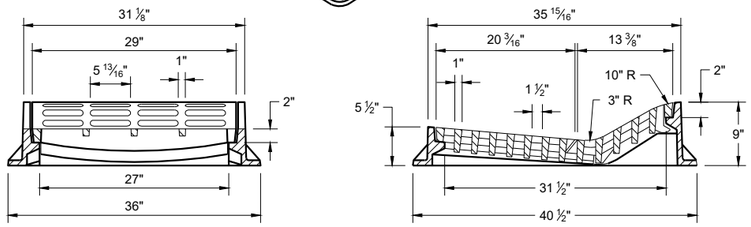
FHWA

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

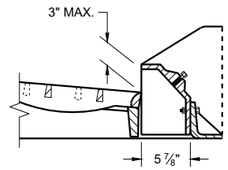
DETAIL DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR INLET COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.





TYPE "F"

USE WITH TYPES "A" AND "D" CONCRETE CURB AND GUTTER, 36"

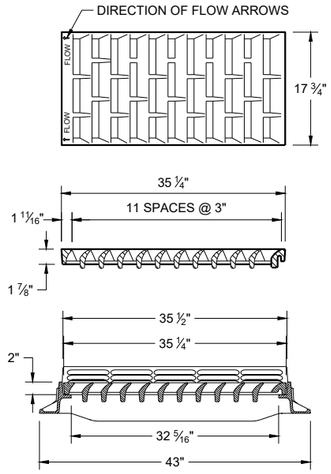


ALTERNATIVE CURB BOX FOR TYPE "HM" COVER

USE WITH TYPES "G" AND "J" CONCRETE CURB AND GUTTER, 30 INCH
NOTED AS TYP "HM-GJ" ON DRAINAGE TABLE

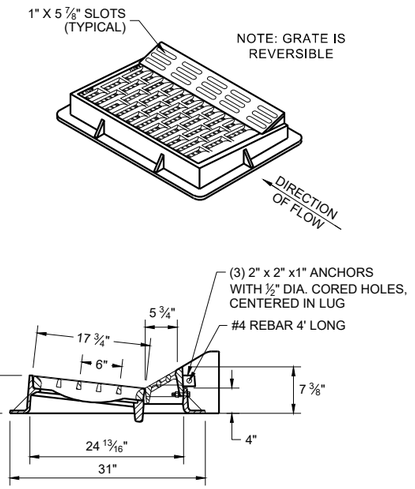
NOTE:
SPECIAL GRATE FOR THE TYPE "H" COVER MAY ALSO BE USED FOR THE TYPE "HM-GJ" COVER.

NOTED AS TYPE HM-GJ-S ON THE DRAINAGE TABLE.



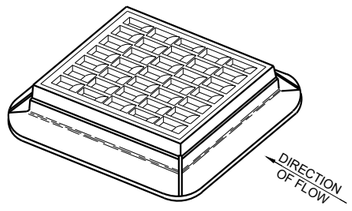
TYPE "HM"

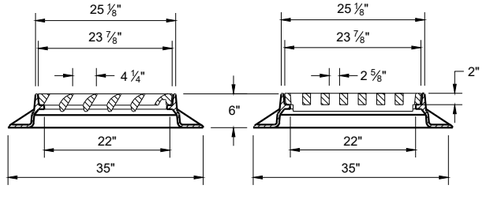
USE WITH TYPES "A" AND "D" CONCRETE CURB AND GUTTER, 36"



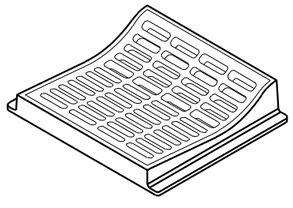
NOTE:
SPECIAL GRATE FOR THE TYPE "H" COVER MAY ALSO BE USED FOR THE TYPE "HM-GJ" COVER.

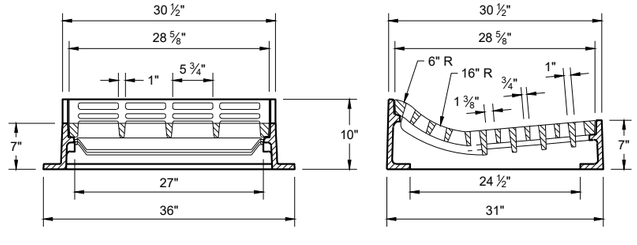
NOTED AS TYPE HM-GJ-S ON THE DRAINAGE TABLE.





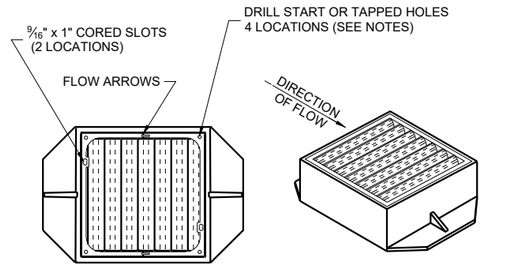
TYPE "S"

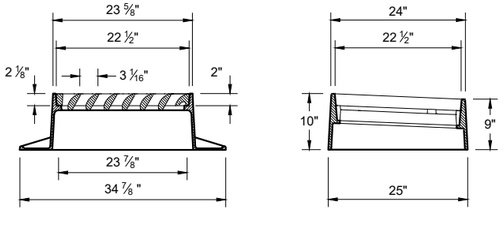




TYPE "T"

USE WITH TYPES "R" AND "T" CONCRETE CURB AND GUTTER, 36"

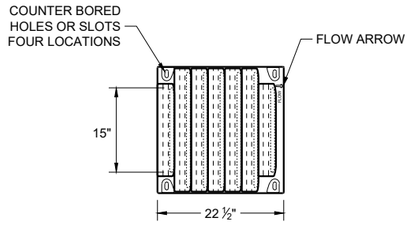




TYPE "V"

NOTES: ALL HARDWARE TO BE SUPPLIED BY CASTING MANUFACTURER ALL DRILLING AND TAPPING GRATES AND FRAMES BY CASTING MANUFACTURER

TYPE V
FRAME - CAST GRAY IRON ASTM A48 CLASS 40A
3/8" DIA. X 1/16" DRILL START IN 4 LOCATIONS
GRATE - CAST GRAY IRON ASTM A-48, CLASS 35B



BOLT DOWN GRATE FOR TYPE "V" COVER

NOTES: ALL HARDWARE TO BE SUPPLIED BY CASTING MANUFACTURER NOTED AS TYPE "V-B" ON DRAINAGE TABLE

TAP 1/2" -13 HOLES IN FOUR LOCATIONS IN FRAME TO BOLT GRATE
FRAME - CAST GRAY IRON ASTM A48 CLASS 40A

GRATE - CAST DUCTILE IRON ASTM A536, 55+KSI YIELD
BOLTS - 1/2" -13 STAINLESS STEEL BOLTS WITH WASHERS
TORQUE BOLTS TO MANUFACTURER SPECIFICATION DO NOT OVERTIGHTEN.

INLET COVERS TYPES F, HM, HM-S, S, T, V, HM-GJ AND HM-GJ-S

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
July 2023 — DATE
/S/ Rodney Taylor — ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR

FHWA

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SDD 08A05-20c

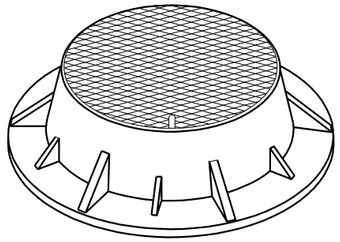
SDD 08A05-20c

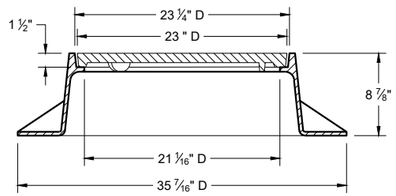
GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

DETAIL DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR MANHOLE COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

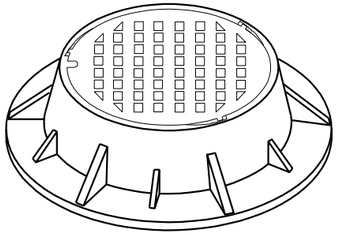
ROUND FRAMES AND COVERS SHALL HAVE CONTINUOUSLY MACHINED BEARING SURFACES TO PREVENT ROCKING AND RATTLING.

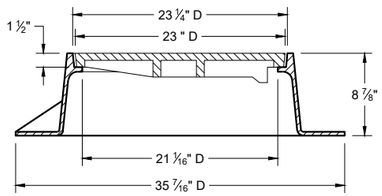




TYPE "J"

NOTE: EITHER CASTING IS ACCEPTABLE

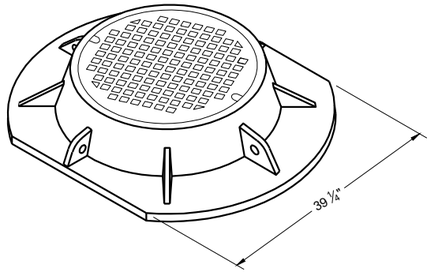


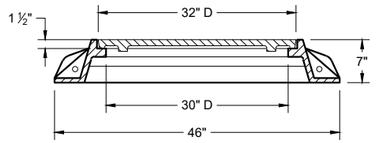


TYPE "J" SPECIAL

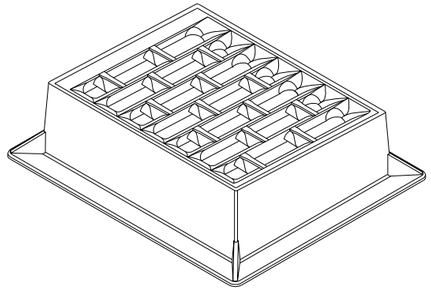
TYPE "B" NON-ROCKING SELF-SEAL LID
(NOTED AS TYPE J-S ON THE DRAINAGE TABLE)

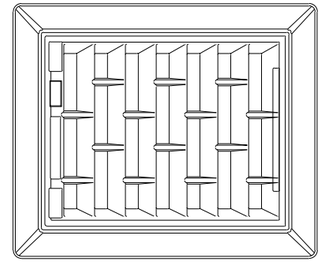
NOTE: EITHER CASTING IS ACCEPTABLE

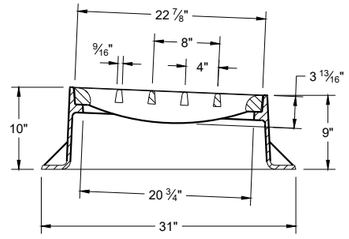


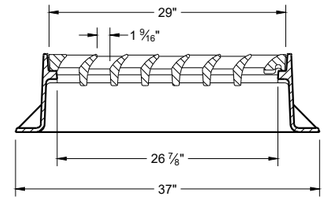


TYPE "K"

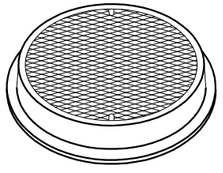


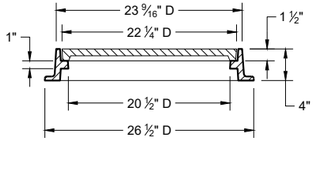




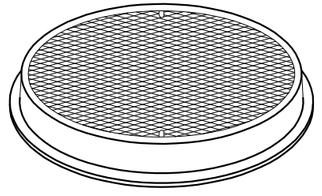


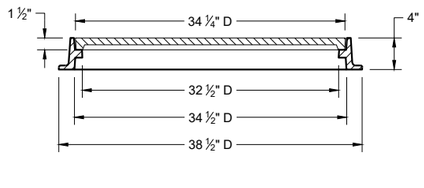
INLET COVER TYPE "BW"





TYPE "L"





TYPE "M"

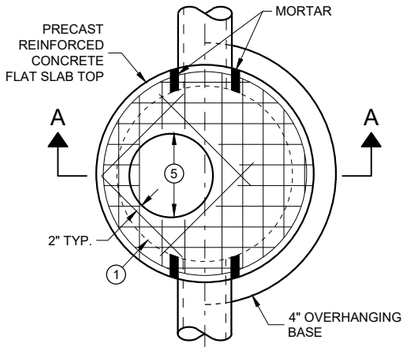
INLET COVERS TYPES BW
MANHOLE COVERS TYPES K, J, J-S, L, AND M

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

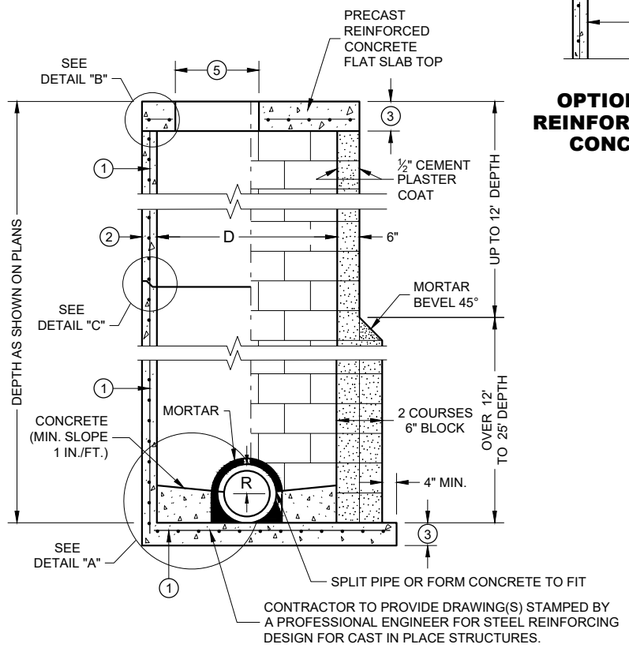
APPROVED
July 2023 — DATE
/S/ Rodney Taylor — ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR

FHWA

6
SDD 08A05-20d



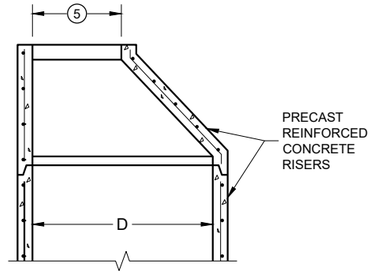
PLAN VIEW
CIRCULAR OPENING



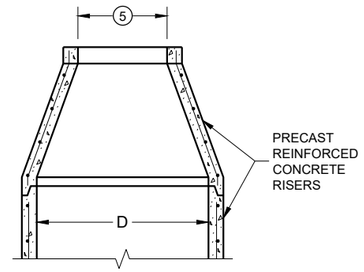
SECTION A - A

PRECAST REINFORCED CONCRETE WITH MONOLITHIC BASE

CONCRETE BLOCK WITH CAST IN PLACE OR PRECAST REINFORCED CONCRETE BASE (1)



OPTIONAL PRECAST REINFORCED CONCRETE ECCENTRIC TOP



OPTIONAL PRECAST REINFORCED CONCRETE CONCENTRIC TOP

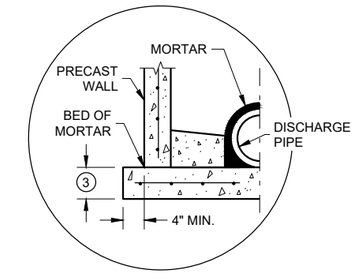
MANHOLE COVER OPENING MATRIX

OPENING SIZE (FT.) \ MANHOLE COVER TYPE	C	ALL J'S	K	L	M
2 DIA.	X	X		X	
3 DIA.			X		X

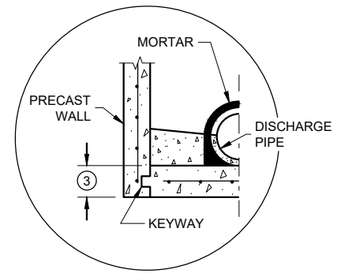
PIPE MATRIX

MANHOLE SIZE (DIA.)	MAXIMUM INSIDE PIPE DIAMETER FOR TWO PIPES		MINIMUM WALL THICKNESS (IN)	MINIMUM PRECAST FLAT SLAB TOP AND BASE THICKNESS
	180° SEPARATION (IN)	90° SEPARATION (IN)		
3-FT	15	12	4	6
4-FT	24	18	4	6
5-FT	36	24	5	8
6-FT	42	36	6	8
7-FT	48	36/42*	7	8
8-FT	60	42	8	8
9-FT	66	54	9	10
10-FT	72	60	10	10

*A 36" PIPE AND A 42" PIPE CAN BE PLACED WITHIN 90 DEGREES. SEE MINIMUM HORIZONTAL PIPE SEPARATION DETAIL.

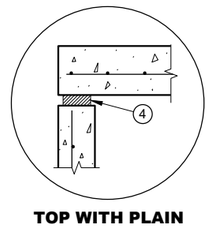


SEPARATE PRECAST REINFORCED CONCRETE BASE OPTION

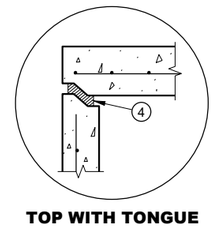


PRECAST REINFORCED CONCRETE WITH INTEGRAL BASE OPTION

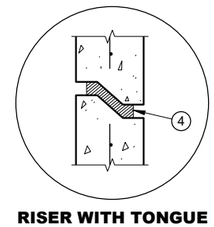
DETAIL "A"



TOP WITH PLAIN END JOINT

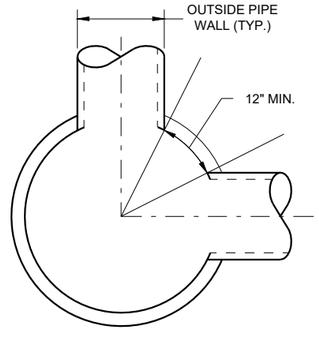


TOP WITH TONGUE AND GROOVE JOINT



RISER WITH TONGUE AND GROOVE JOINT

DETAIL "B" **DETAIL "C"**



MINIMUM HORIZONTAL PIPE SEPARATION

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST MANHOLE UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL DRAINAGE STRUCTURES ARE DESIGNATED ON THE PLANS AS "MANHOLES 3X3-L", "CATCH BASINS 4-B", "INLETS 2X3-H", ETC. THE FIRST NUMBERS DESIGNATE THE SIZE OF THE STRUCTURE, AND THE FOLLOWING LETTER DESIGNATES THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF FOUNDATION BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

PRECAST REINFORCED CONCRETE CONE TOPS (ECCENTRIC OR CONCENTRIC) OR PRECAST REINFORCED CONCRETE FLAT SLAB TOPS MAY BE USED ON CONCRETE BLOCK STRUCTURES.

ECCENTRIC CONE TOPS MAY BE USED ON ALL STRUCTURES. CONCENTRIC CONE TOPS SHALL BE USED ONLY ON STRUCTURES 5 FEET OR LESS IN DEPTH UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

STEPS MEETING AASHTO M199 AND THE FOLLOWING REQUIREMENTS SHALL BE INSTALLED IN ALL STRUCTURES OVER 5 FEET IN DEPTH: 16 INCH C-C MAXIMUM SPACING; PROJECT A MINIMUM CLEAR DISTANCE OF 4 INCHES FROM THE WALL AT THE POINT OF EMBEDMENT; MINIMUM LENGTH OF 10 INCHES; MINIMUM WALL EMBEDMENT OF 3 INCHES. FERROUS METAL STEPS NOT PAINTED OR TREATED TO RESIST CORROSION SHALL HAVE A MINIMUM CROSS SECTIONAL DIMENSION OF 1 INCH.

STEPS OF APPROVED POLYPROPYLENE PLASTIC COATED REINFORCEMENT BAR ARE ACCEPTABLE. REINFORCING BAR MUST BE A MINIMUM OF ½ INCH AND MEET THE REQUIREMENTS OF ASTM A615.

CERTIFICATION SHALL BE PROVIDED THAT INSTALLED STEPS WHEN TESTED IN ACCORDANCE WITH SECTION 10 OF AASHTO T280 CAN WITHSTAND A VERTICAL LOAD OF 800 LBS. AND A HORIZONTAL LOAD OF 400 LBS.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

ALL PRECAST MANHOLE UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF AASHTO DESIGNATION M199.

PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

CONCRETE BLOCK WILL NOT BE PERMITTED FOR STRUCTURES GREATER THAN 4 FEET IN DIAMETER.

4" OVERHANGING BASES ARE REQUIRED FOR ALL CONCRETE BLOCK INSTALLATIONS. 4" OVERHANG IS REQUIRED WHEN SEPARATE PRECAST BASE IS PROVIDED. OVERHANG IS NOT REQUIRED ON PRECAST STRUCTURES WITH AN INTEGRAL OR MONOLITHIC BASE.

FOR ADDITIONAL CONFIGURATIONS, MAINTAIN A MINIMUM OF 12 INCHES AS MEASURED FROM THE INSIDE OF THE STRUCTURE WALL BETWEEN THE OUTSIDE PIPE WALLS OF ADJACENT PIPES. SEE DETAIL "D".

(1) FOR PRECAST MANHOLES PROVIDE REINFORCING STEEL IN ACCORDANCE TO AASHTO M199.

(2) SEE PIPE MATRIX TABLE FOR MINIMUM WALL THICKNESS FOR PRECAST MANHOLES

(3) SEE PIPE MATRIX TABLE FOR MINIMUM THICKNESS OF PRECAST FLAT SLAB TOPS AND BASES.

(4) JOINTS TO BE SEALED WITH A BUTYL RUBBER SEAL PER SEALANT MANUFACTURERS RECOMMENDATIONS CONFORMING TO ASTM C 990 (TYP.).

(5) SEE MANHOLE COVER OPENING MATRIX.

MANHOLES 3-FT, 4-FT, 5-FT, 6-FT, 7-FT, 8-FT, 9-FT AND 10-FT DIAMETER

MANHOLES, 3-FT, 4-FT 5-FT, 6-FT, 7-FT, 8-FT, 9-FT AND 10-FT DIAMETER

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2021 — DATE
/S/ Rodney Taylor — ROADWAY STANDARDS DEVE ENGINEER

FHWA

SDD 08B09 - 03

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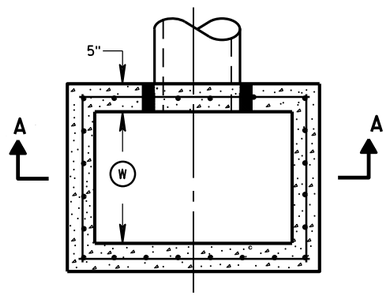
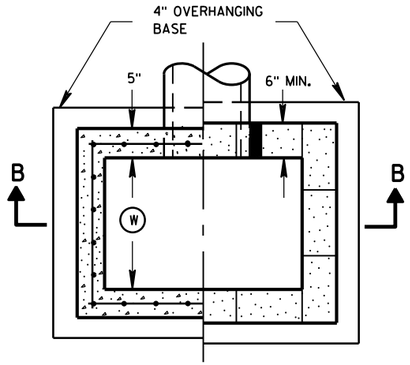
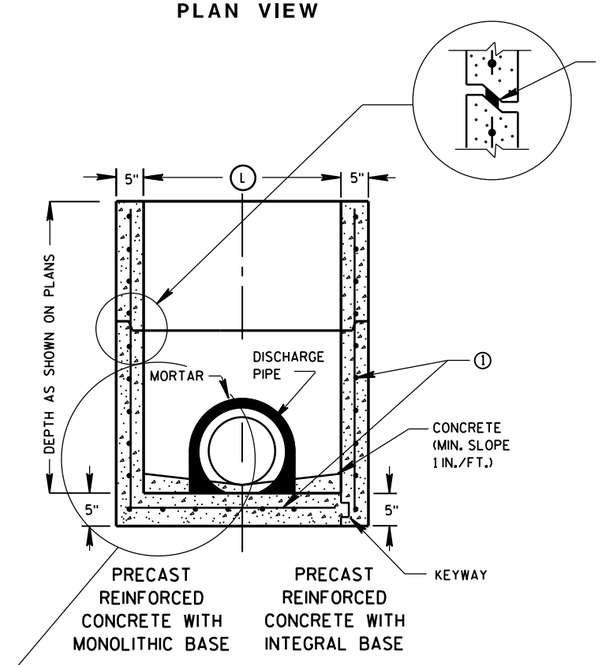
SDD 08B09 - 03



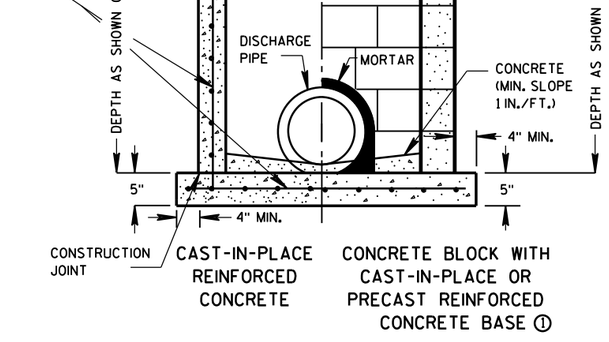
PLAN VIEW



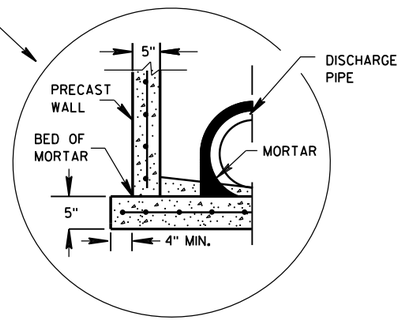
PLAN VIEW



SECTION A-A



SECTION B-B



SEPARATE PRECAST REINFORCED CONCRETE BASE OPTION

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST INLET UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL PRECAST INLET UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF ASTM C 913.

ALL DRAINAGE STRUCTURES ARE DESIGNATED ON THE PLANS AS "MANHOLES 3X3-L", "CATCH BASINS 4-B", "INLETS 2X3-H", ETC. THE FIRST NUMBERS DESIGNATES THE SIZE OF THE STRUCTURE, AND THE FOLLOWING LETTER DESIGNATES THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF FOUNDATION BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

4" OVERHANGING BASES ARE REQUIRED FOR CAST-IN-PLACE REINFORCED CONCRETE AND CONCRETE BLOCK INSTALLATIONS.
4" OVERHANG IS REQUIRED WHEN SEPARATE PRECAST BASE IS PROVIDED.
OVERHANG IS NOT REQUIRED ON PRECAST STRUCTURES WITH AN INTEGRAL OR MONOLITHIC BASE.

MAXIMUM INSIDE PIPE DIAMETER DETERMINED BY 3 INCH CLEARANCE ON EACH SIDE OF THE OUTSIDE WALL OF THE PIPE. SEE DETAIL "A". ASSUMES PIPE ENTERS PERPENDICULAR TO THE STRUCTURE.

① FOR PRECAST INLETS PROVIDE REINFORCING STEEL IN ACCORDANCE TO ASTM C 913.

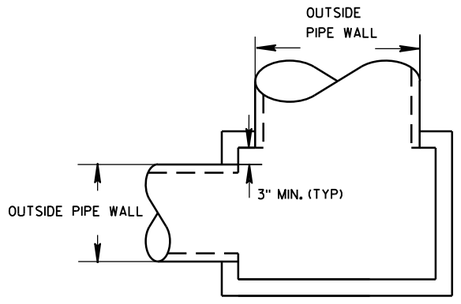
② CONTRACTOR TO PROVIDE DRAWING(S) STAMPED BY A PROFESSIONAL ENGINEER FOR STEEL REINFORCING DESIGN FOR CAST-IN-PLACE STRUCTURES.

INLET COVER MATRIX

INLET SIZE		INLET COVER TYPE	ALL A'S	ALL B'S	BW	F	ALL H'S	S	T	V	WM
	WIDTH (W) (FT)	LENGTH (L) (FT)									
2X2-FT	2	2	X	X				X		X	
2X2.5-FT	2	2.5			X			X	X	X	X
2X3-FT	2	3					X				
2.5X3-FT	2.5	3				X					

PIPE MATRIX

INLET SIZE	MAXIMUM INSIDE PIPE DIAMETER	
	WIDTH (IN)	LENGTH (IN)
2X2-FT	12	12
2X2.5-FT	12	18
2X3-FT	12	24
2.5X3-FT	18	24



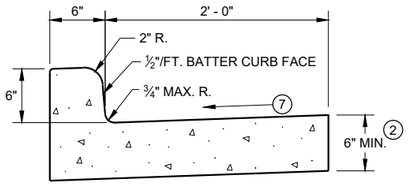
DETAIL "A"

INLETS 2X2-FT, 2X2.5-FT, 2X3-FT AND 2.5X3-FT

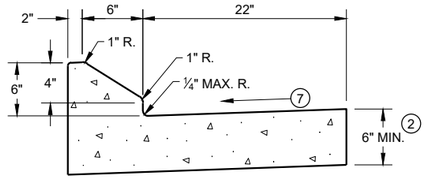
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
Sept., 2016 — DATE
/S/ Rodney Taylor
ROADWAY STANDARDS D IT UNIT SUPERVIS
FHWA

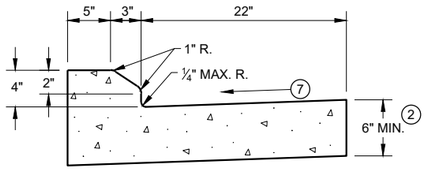
INLETS 2X2-FT, 2X2.5-FT, 2X3-FT AND 2.5X3-FT



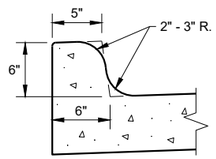
TYPES A (1) & D



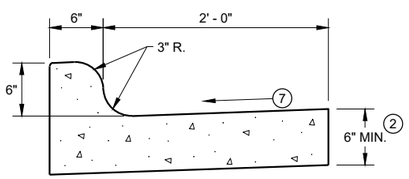
6" SLOPED CURB TYPES G (1) & J



4" SLOPED CURB TYPES G (1) & J

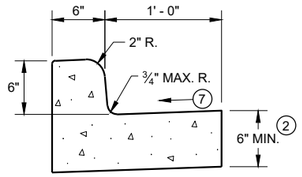


TYPES K (1) & L
(OPTIONAL CURB SHAPE)



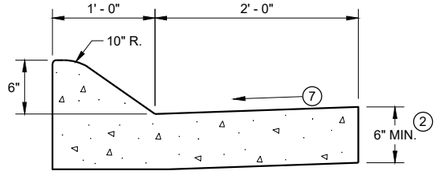
TYPES K (1) & L

CONCRETE CURB AND GUTTER 30"

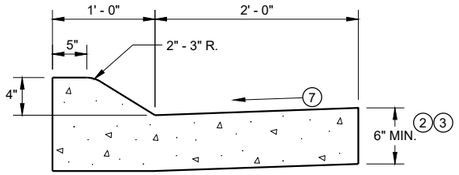


TYPES A (1) & D

CONCRETE CURB AND GUTTER 18"

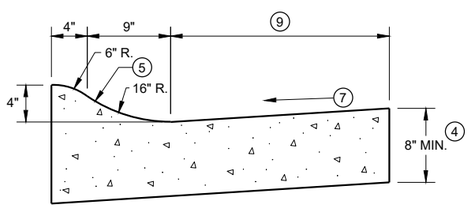


6" SLOPED CURB TYPES A (1) & D



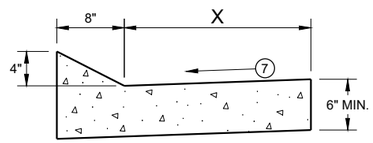
4" SLOPED CURB TYPES A (1) & D

CONCRETE CURB AND GUTTER 36"



4" SLOPED CURB TYPES R (1) & T

TBT & TBTT	X
30"	22"
36"	28"

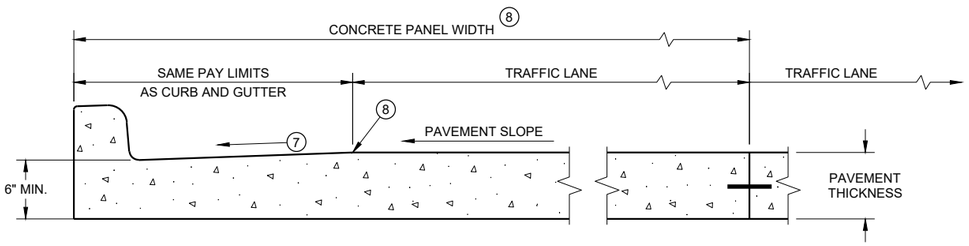


TYPES TBT & TBTT (1)

CONCRETE CURB AND GUTTER

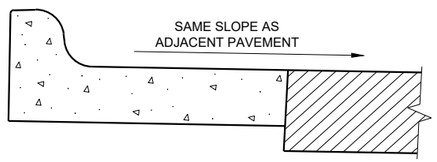
PAVEMENT THICKNESS AND MAXIMUM CONCRETE PANEL WIDTH TABLE

PAVEMENT THICKNESS	MAXIMUM PANEL WIDTH
LESS THAN 10"	12'
10" & ABOVE	15'



PARTIAL SECTION OF PAVEMENT * WITH INTEGRAL CURB AND GUTTER

* BIKE LANE IS NOT SHOWN



REVERSE SLOPE GUTTER (6)
(TYPICAL FOR ALL CURB & GUTTER TYPES)

GENERAL NOTES

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

INTEGRAL CURB AND GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB AND GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

(1) TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.

(2) THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.

(3) USE 8" MINIMUM GUTTER THICKNESS WHEN USED WITH AN ADJACENT CONCRETE TRUCK APRON PLACED BEHIND BACK OF CURB.

(4) THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.

(5) UNLESS OTHERWISE NOTED, FOR STAKING PURPOSES THE FACE OF CURB IS 6" FROM THE BACK OF CURB.

(6) WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.

(7) USE 4% GUTTER CROSS SLOPE UNLESS OTHERWISE NOTED IN THE PLANS.

(8) INCLUDE LONGITUDINAL JOINT AND TIE BARS ALONG LANE EDGE WHEN CONCRETE PANEL WIDTH EXCEEDS THE MAXIMUM WIDTH PER TABLE BELOW. LONGITUDINAL JOINT(S) ARE NOT ALLOWED WITHIN TRAFFIC LANES AND BIKE LANES. LONGITUDINAL JOINT MAY BE SAWED.

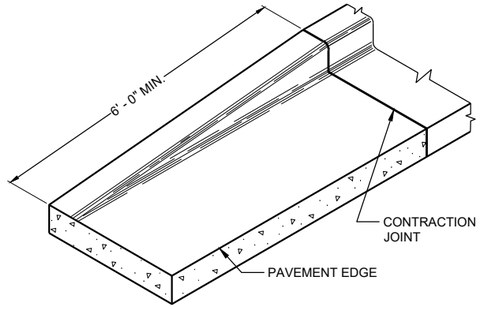
(9) CONCRETE CURB AND GUTTER 4-INCH SLOPED 30-INCH TYPE "R" AND "T" = 17 INCHES
CONCRETE CURB AND GUTTER 4-INCH SLOPED 36-INCH TYPE "R" AND "T" = 23 INCHES

CONCRETE CURB AND GUTTER

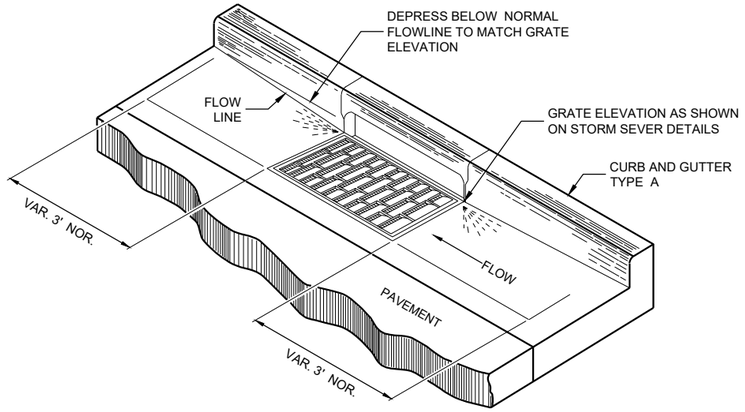
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

SDD 08D01 - 23a



END SECTIONCURB AND GUTTER



DETAIL OF CURB AND GUTTER AT INLETS

(TYPICAL H INLET COVER SHOWN)

GENERAL NOTES

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

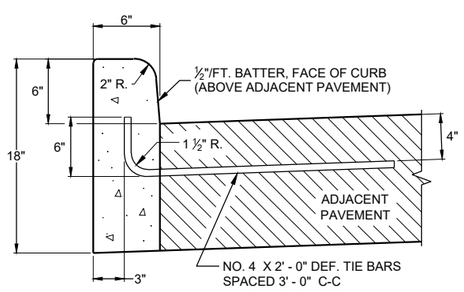
UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

(1) TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.

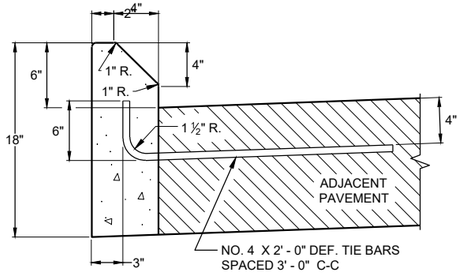
(2) THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.

(10) REFER TO SDD 08D18 AND 08D19 FOR ADDITIONAL DRIVEWAY ENTRANCE CURB DETAILS.

(11) PLACE 1" THICK EXPANSION JOINT MATERIAL BETWEEN VERTICAL FACE CURB TYPES EXTENDING FROM THE TOP OF CURB TO 1 INCH BELOW THE ADJOINING CONCRETE SURFACE. RIGID CONCRETE STRUCTURES INCLUDE RAISED CONCRETE MEDIANS, CONCRETE SAFETY ISLANDS, SPLITTER ISLANDS, OR LOCATIONS IDENTIFIED ON THE PLANS.

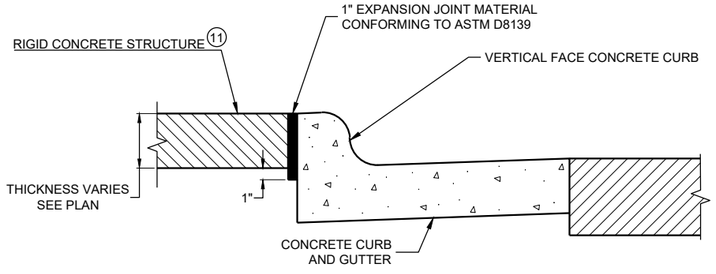


TYPES A(1) & D

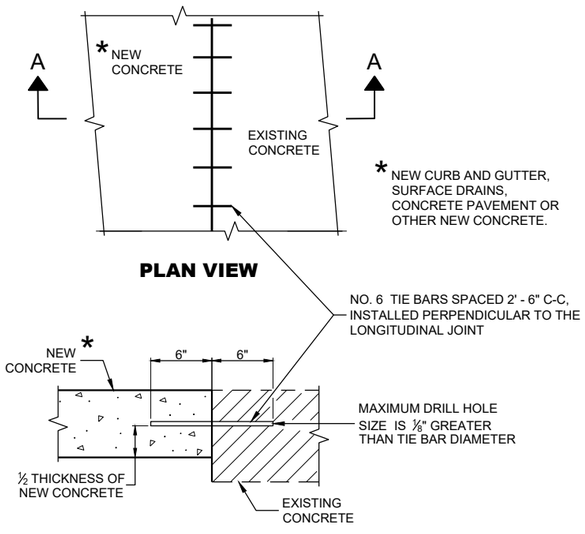


TYPES G(1) & J

CONCRETE CURB



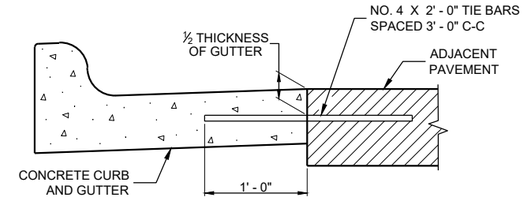
EXPANSION JOINT DETAIL FOR VERTICAL CURB ABUTTING A RIGID STRUCTURE(11)



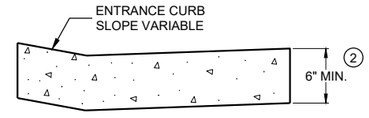
PLAN VIEW

SECTION A - A

TIE BARS DRILLED INTO EXISTING PAVEMENT



TYPICAL TIE BAR LOCATION(1)



DRIVEWAY ENTRANCE CURB(10)

(WHEN DIRECTED BY THE ENGINEER)

CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 — DATE
/S/ Rodnery Taylor — ROADWAY STANDARDS DEVE ENGINEER

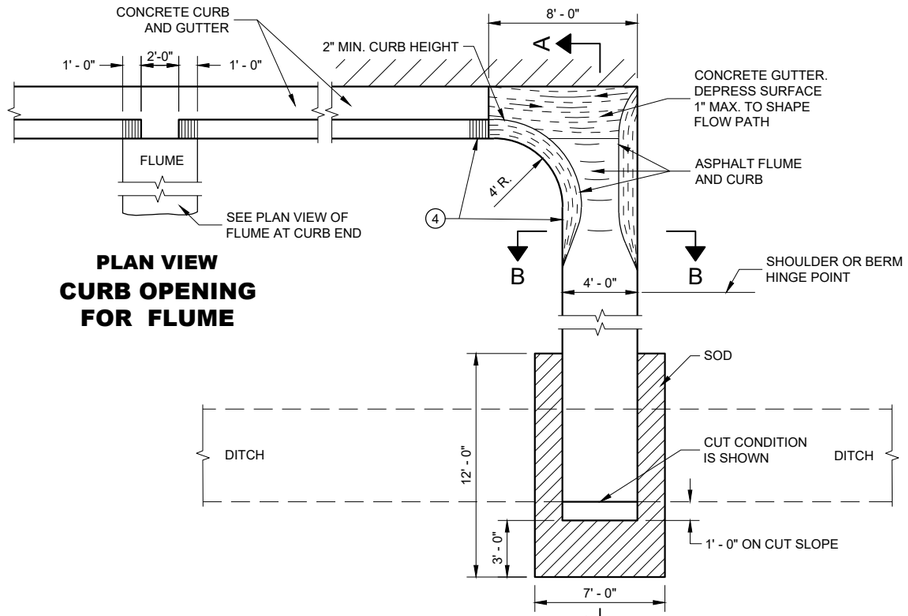
FHWA

6

SDD08D01 - 23b

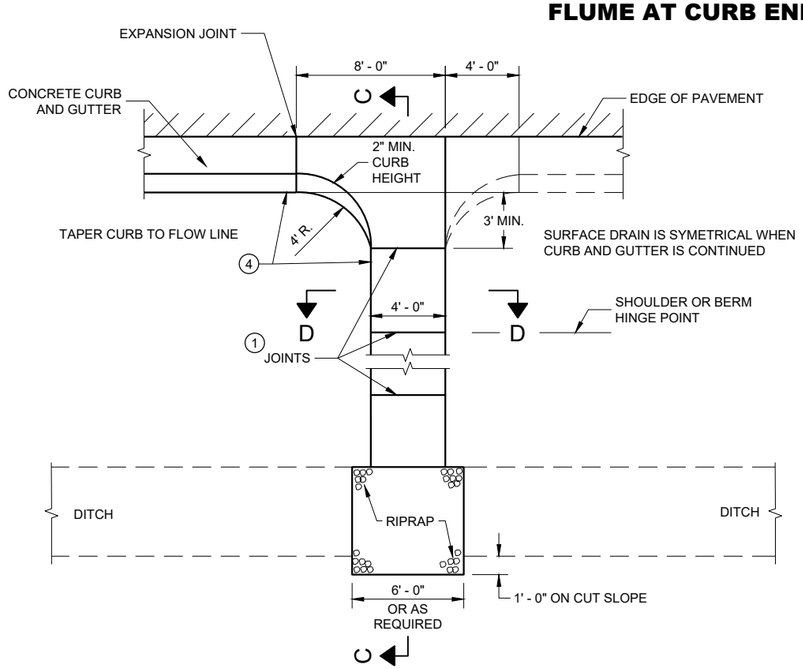
NOTE: TAPER CURB ENDS TO GUTTER IN 1' - 0"

ASPHALTIC FLUME



PLAN VIEW
CURB OPENING FOR FLUME

PLAN VIEW
FLUME AT CURB END



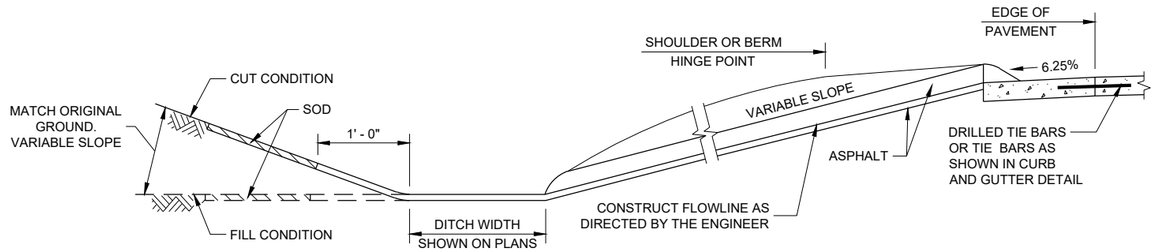
PLAN VIEW
CONCRETE SURFACE DRAIN

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

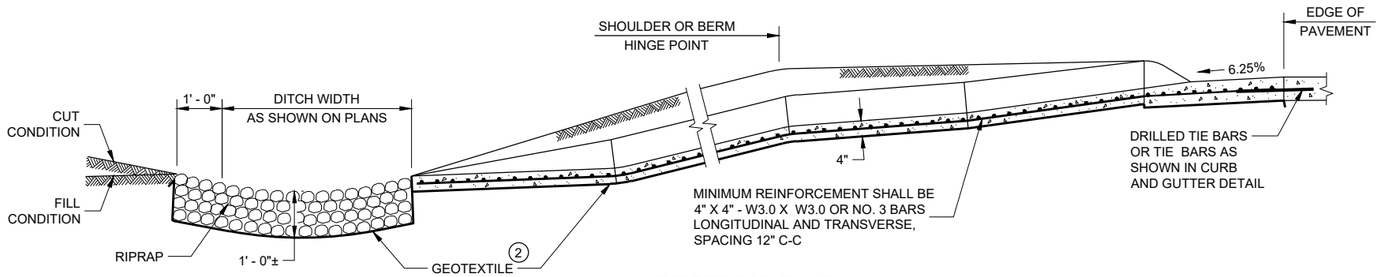
4" X 4" - W3.0 X W3.0 CONCRETE REINFORCEMENT SHALL BE IN ACCORDANCE WITH AASHTO SPECIFICATION M55.

1. JOINTS SHALL BE ⅛" TO ¼" WIDE BY 1½" DEEP AND SPACED AT UNIFORM INTERVALS OF APPROXIMATELY 4 FEET.
2. GEOTEXTILE TYPE "R" SHALL UNDERLAY THE FULL LENGTH AND WIDTH OF THE CONCRETE SURFACE DRAIN AND RIPRAP.
3. CONCRETE SURFACE DRAIN WITHOUT CURB AND GUTTER MAY BE USED ON BACKSLOPES WHEN SPECIFIED.
4. ANGLE OF FLUME IN RELATION TO BACK OF CURB TO BE CONSTRUCTED PER THE PLAN DETAILS OR AS DIRECTED BY THE ENGINEER. ANGLE OF FLUME MAY BE OTHER THAN 90 DEGREES AS SHOWN.

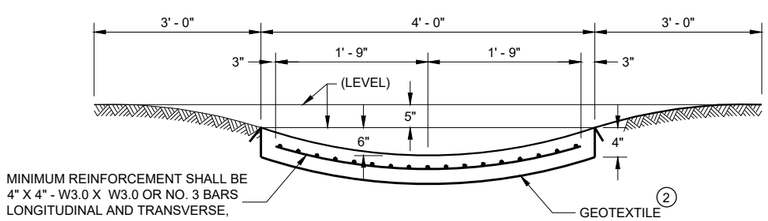


SECTION A - A

SECTION B - B



SECTION C - C



SECTION D - D

CONCRETE SURFACE DRAINS AND ASPHALTIC FLUMES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023
DATE

/S/ Rodney Taylor
ROADWAY STANDARDS DEVE ENGINEER

FHWA

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SDD 08D04 - 07

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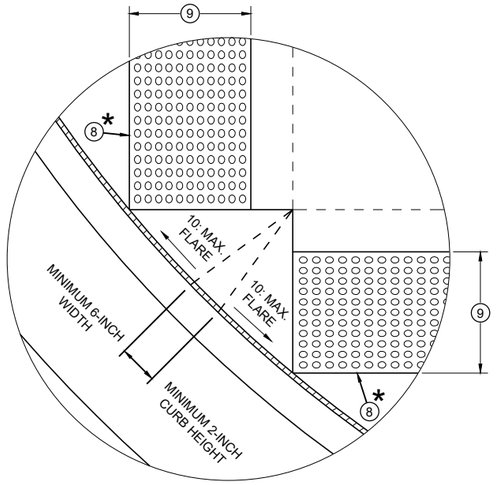
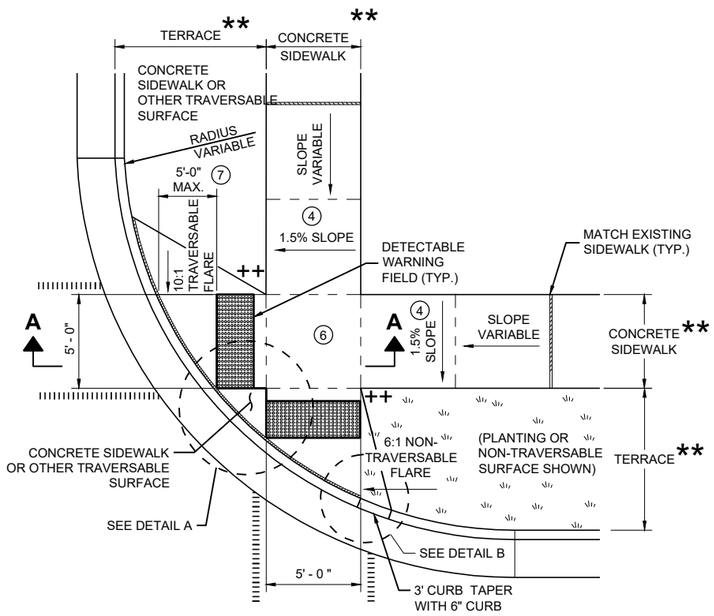
SDD 08D04 - 07



DETAIL A



PLAN VIEW
CURB RAMP TYPE 2
(CENTER OF CORNER RADIUS)

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE SHALL BE FROM THE SAME MANUFACTURER.

(2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN ¼ - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.

(3) AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE (2.67% OR LESS) AND NOT TO EXCEED 11% GRADE CHANGE.

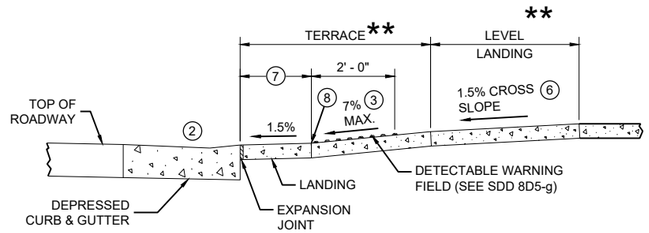
(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

(6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET X 5 FEET.

(7) WHEN GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.

(8) PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

(9) WHEN DISTANCE IS LESS THAN 6' - 0", IT MAY BE DIFFICULT TO ACHIEVE A 7% SLOPE OR FLATTER ALONG THE RAMP. REDUCE CURB HEIGHT IN TRIANGLE AREA TO ACHIEVE 7% SLOPE OR FLATTER ON RAMP. CONSTRUCT 2-INCH MINIMUM CURB HEIGHT BETWEEN 10:1 FLARES.

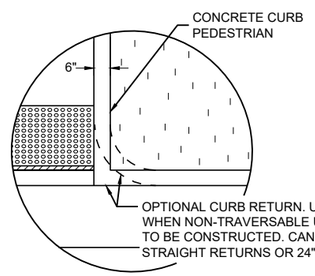


SECTION A - A FOR TYPE 2

* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK

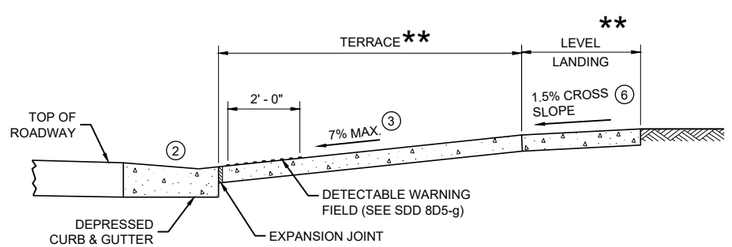
** WIDTH SHOWN ELSEWHERE IN THE PLANS

++ CONSTRUCT 6" WEDGE TO AVOID CONCRETE BREAKAGE



DETAIL B

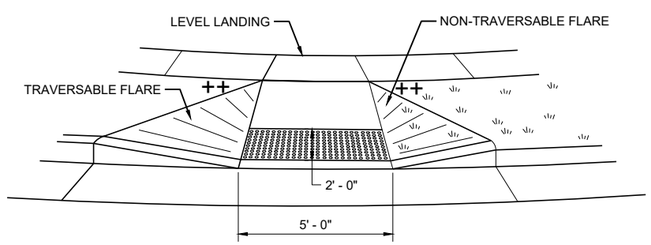
PLAN VIEW
CURB RAMP TYPE 3
(OUTSIDE OF CROSSWALK AREA)



SECTION B - B FOR TYPE 3

LEGEND

- ½" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT SIDEWALK
- PAVEMENT MARKING CROSSWALK (WHITE)



VIEW C - C FOR TYPE 3

CURB RAMPS TYPE 2 AND 3

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

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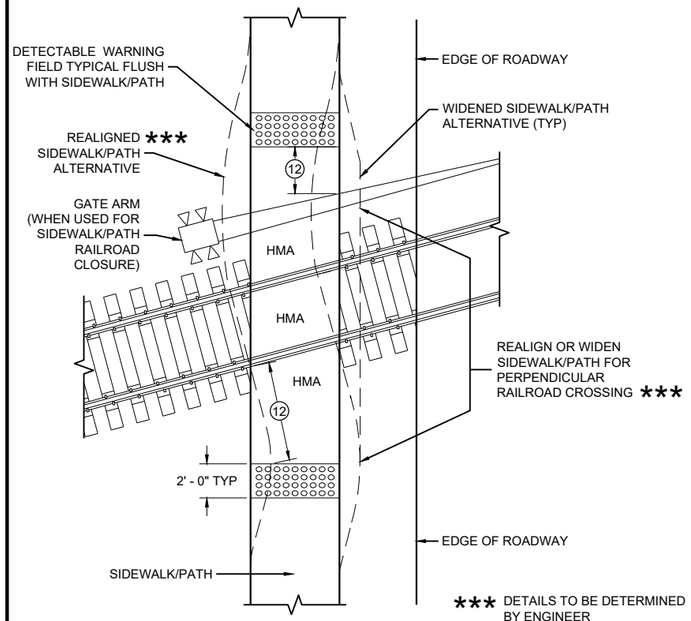
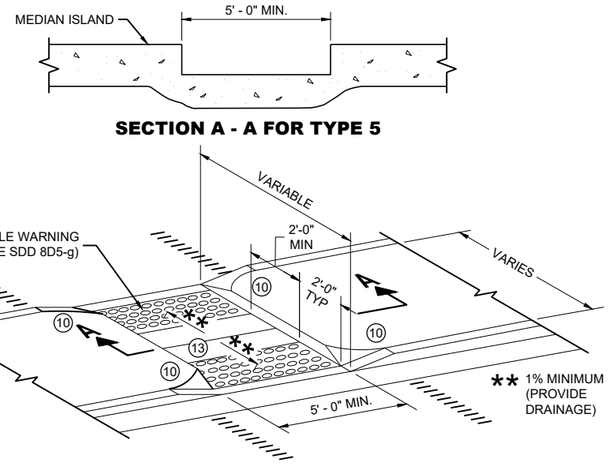
SDD 08D05-21b



CURB RAMP TYPE 8

DETECTABLE WARNINGS FOR SIDEWALKS OR SHARED USE PATHS AT RAILROAD CROSSINGS



SECTION A - A FOR TYPE 5

CURB RAMP TYPE 5

MEDIAN ISLAND NON-ELEVATED PEDESTRIAN CROSSING

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

(2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN ¼ - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.

(3) AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.

(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

(6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.

(10) INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

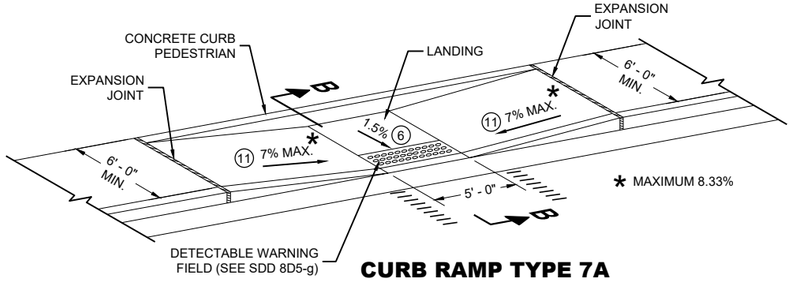
(11) SLOPE SIDEWALK TOWARD LANDING AS SHOWN WHERE THERE IS NO TERRACE OR WHERE THE TERRACE WIDTH IS LESS THAN 6 FEET WIDE.

(12) THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO A RAILROAD CROSSING SHALL BE 1.5 FEET ±0.1' FROM THE FACE OF THE GATE ARM IF THE GATE ARM EXTENDS ACROSS THE SIDEWALK/PATH. WHERE THERE IS NO PEDESTRIAN GATE, THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO THE RAILROAD TRACK IS 15 FEET MAXIMUM AND 12 FEET MINIMUM, 15 FEET TYPICAL FROM THE NEAREST RAIL.

(13) DO NOT INSTALL DETECTABLE WARNING FIELDS AT THE EDGES OF STEET-LEVEL PEDESTRIAN REFUGE ISLANDS IF A MINIMUM 2 FOOT CONCRETE SURFACE WITHOUT DETECTABLE WARNINGS (MEASURED IN THE DIRECTION OF PEDESTRIAN TRAVEL) CANNOT BE ACHIEVED.

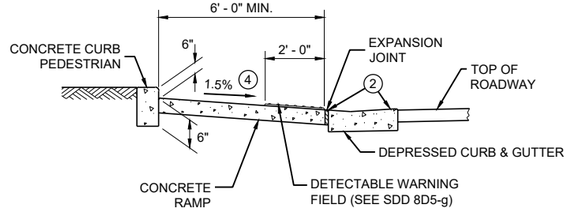
LEGEND

- ½" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)

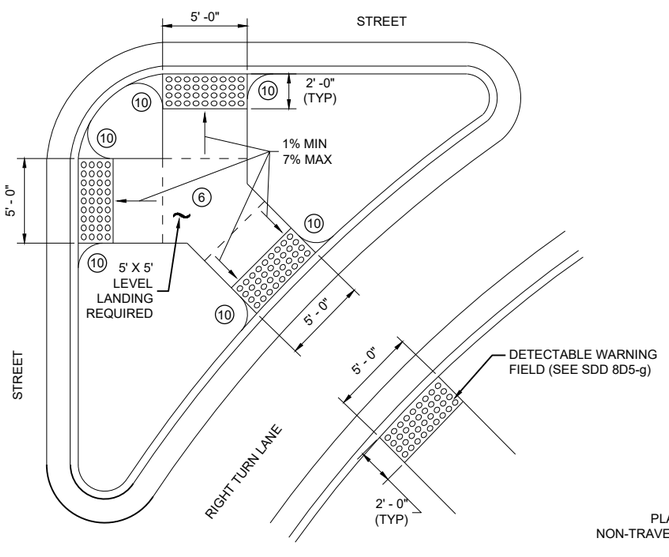


CURB RAMP TYPE 7A

FOR INTERSECTIONS AND MID BLOCK CROSSINGS



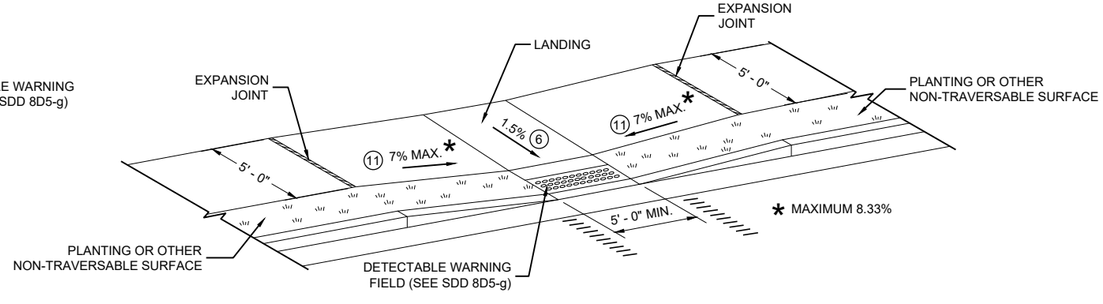
SECTION B - B FOR TYPE 7A



CURB RAMP TYPE 6

DETECTABLE WARNING AT ISLANDS

REFER TO GENERAL NOTES (2) AND (3) FOR ALL ISLAND CURB RAMPS



CURB RAMP TYPE 7B

FOR INTERSECTIONS AND MID BLOCK CROSSINGS

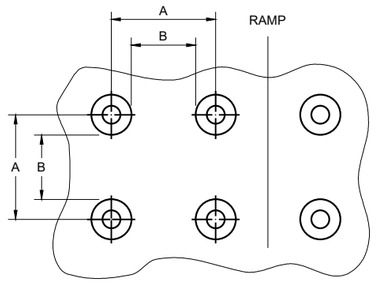
CURB RAMPS TYPE 5, 6, 7A, 7B & 8

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

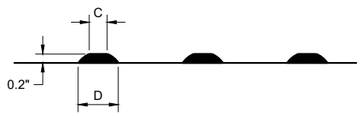
SDD 08D05-21e

	MIN.	MAX.
A	1.6"	2.4"
B	0.65"	1.5"
C	*	*
D	0.9"	1.4"

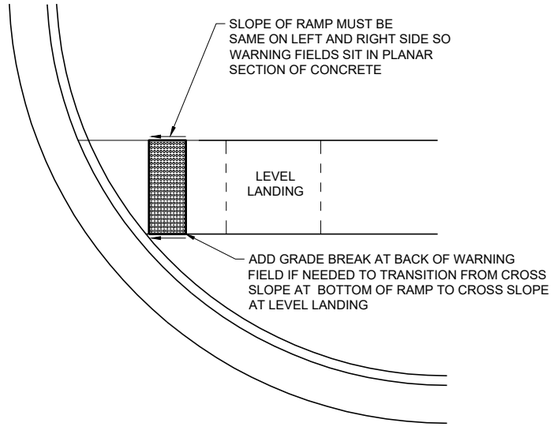
* THE C DIMENSION IS 50% TO 65% OF THE D DIMENSION.



PLAN VIEW



ELEVATION VIEW



DETECTABLE WARNING FIELD
PLANAR INSTALLATION

GENERAL NOTES

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AT A CURB RAMP SHALL BE FROM THE SAME MANUFACTURER.

PLACE ALL DETECTABLE WARNING FIELD SYSTEMS IN ACCORDANCE TO THE MANUFACTURER'S RECOMMENDATION.

FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FILED ARE PROHIBITED.

DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.

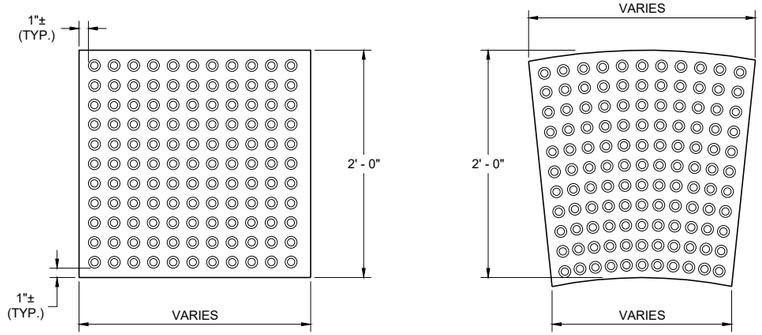
FOR RADIAL DETECTABLE WARNING FIELD APPLICATIONS WHERE STANDARD RADIAL PLATES ARE NOT AVAILABLE AT AN INTERSECTION CURB RADIUS, A COMBINATION OF SQUARE OR RECTANGULAR PLATES AND RADIAL PLATES MAY BE USED TO FORM RADIAL CONFIGURATION. RADIAL WEDGE PLATES IN COMBINATION WITH SQUARE PLATES ARE ALSO ACCEPTABLE. FOLLOW MANUFACTURER'S RECOMMENDATIONS.

REFER TO CONTRACT AND STANDARD SPECIFICATIONS FOR FIELD CUTTING REQUIREMENTS.

DO NOT EMBED IN CONCRETE ANY FIELD-CUT PLATES WITH CUT EDGES SHORTER THAN 6 INCHES. CONSULT WITH MANUFACTURER FOR RE-DRILLING AND ANCHORING REQUIREMENTS OF FIELD-CUT PLATES.

(15) FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN ⅛" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.

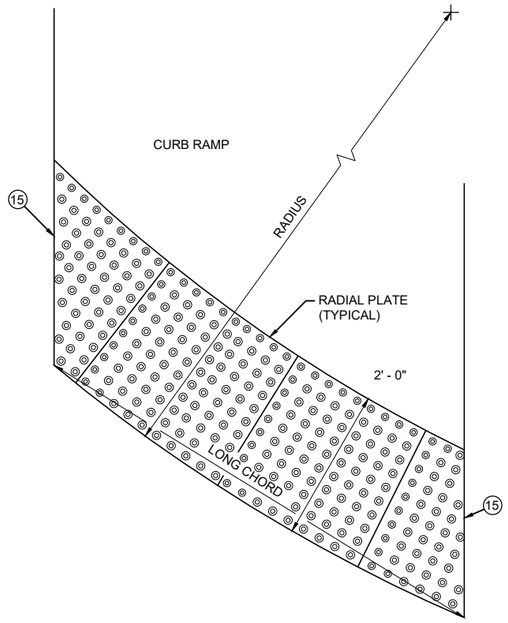
TRUNCATED DOMES
DETECTABLE WARNING PATTERN DETAIL



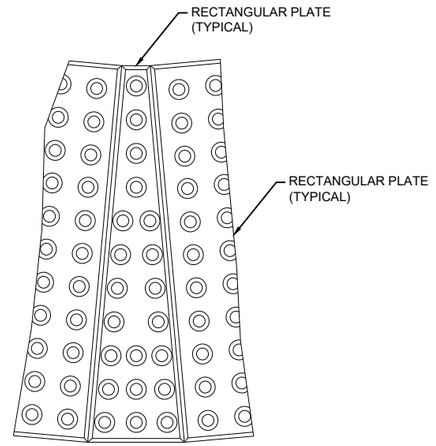
RECTANGULAR PLATES

RADIAL PLATES

PLAN VIEW
DETECTABLE WARNING FIELDS (TYPICAL)



PLAN VIEW
RADIAL DETECTABLE WARNING FIELD ATTRIBUTES



PLAN VIEW
RADIAL WEDGE PLATE CONNECTION DETAIL

CURB RAMPS
RECTANGULAR AND RADIAL DETECTABLE WARNING PLATES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
July 2023 — DATE
/S/ Rodney Taylor — ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR

FHWA

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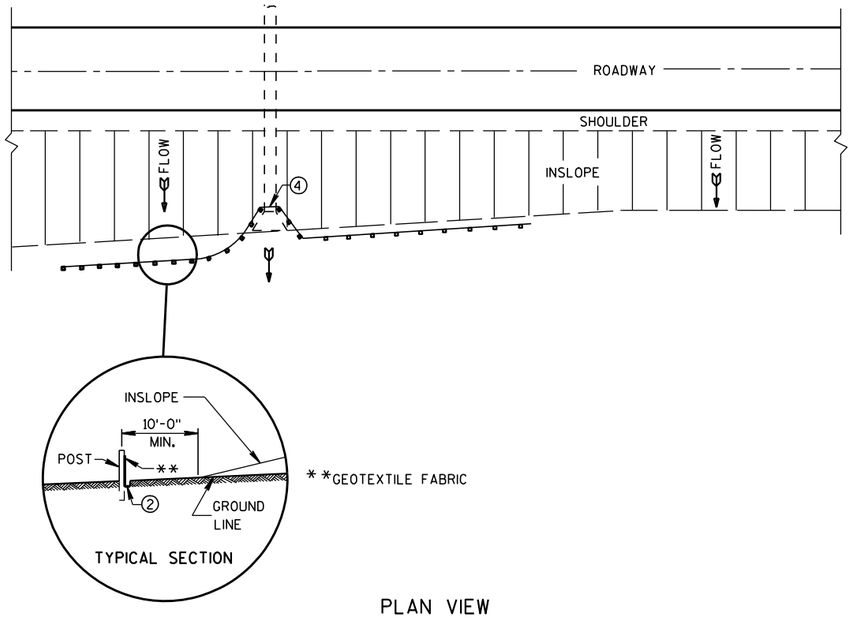
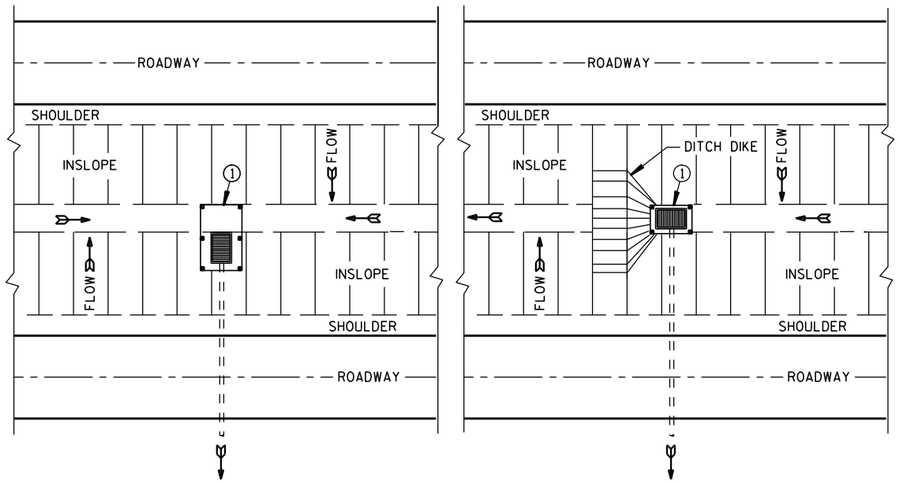
SDD 08D05-21g



PLAN VIEW

TYPICAL APPLICATION OF SILT FENCE



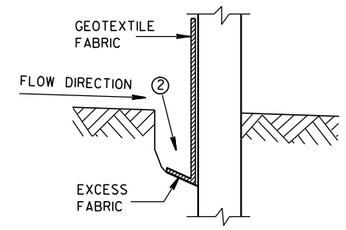
PLAN VIEW

SILT FENCE AT MEDIAN SURFACE DRAINS

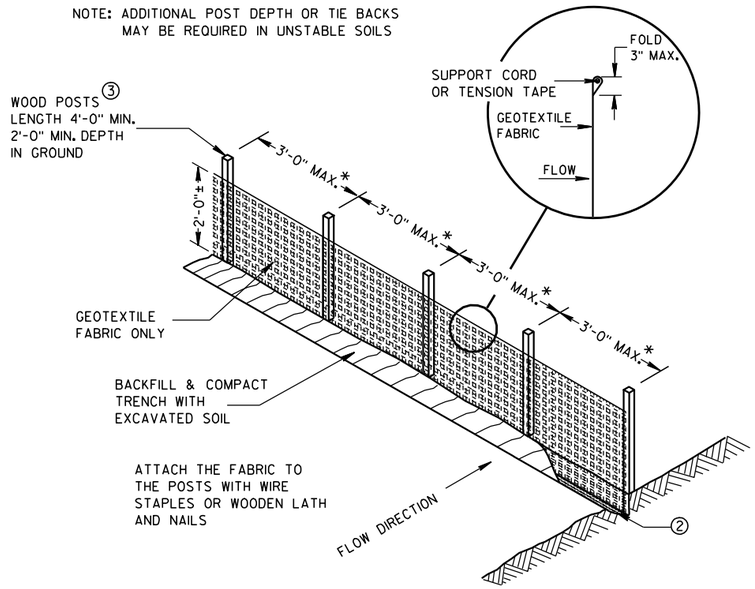
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

1. HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
2. FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
3. WOOD POSTS SHALL BE A MINIMUM SIZE OF 1⅛" X 1⅛" OF OAK OR HICKORY.
4. SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
5. CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.

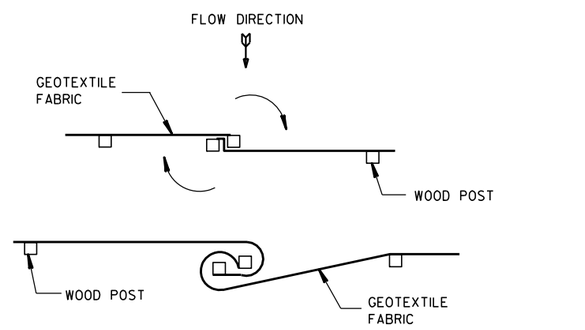


TRENCH DETAIL

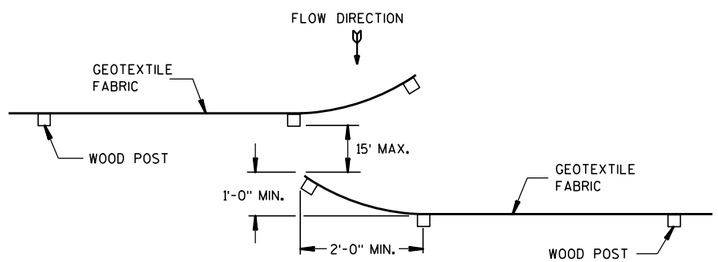


*NOTE: 8'-0" POST SPACING ALLOWED IF A WOVEN GEOTEXTILE FABRIC IS USED.

SILT FENCE

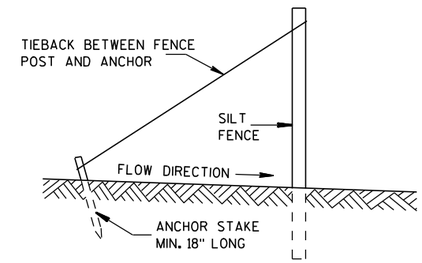


TWIST METHOD



HOOK METHOD

JOINING TWO LENGTHS OF SILT FENCE 5



SILT FENCE TIE BACK

(WHEN REQUIRED BY THE ENGINEER)

SILT FENCE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
4-29-05 DATE
/S/ Beth Cannestra
CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA

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S.D.D. 8 E 9-6

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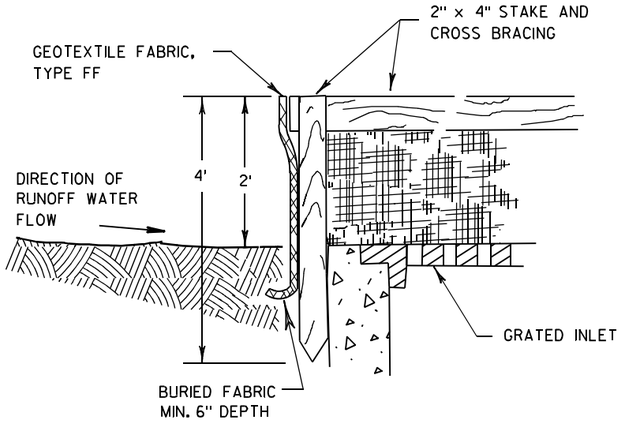
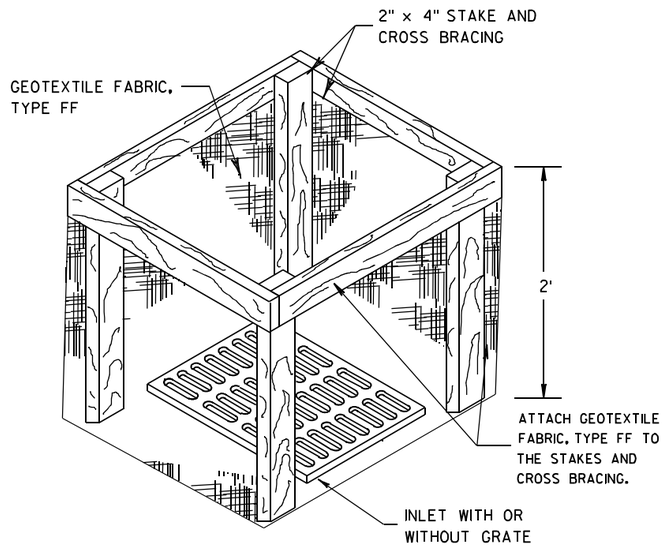
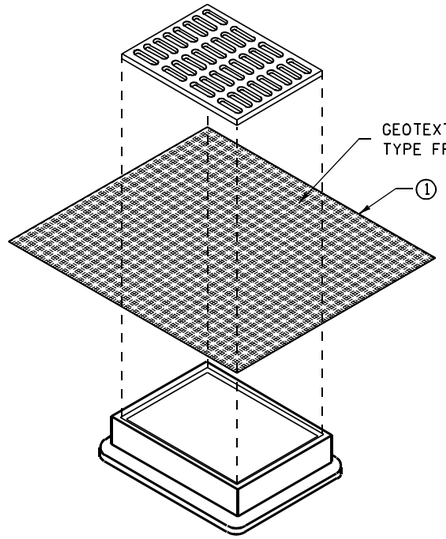
S.D.D. 8 E 9-6



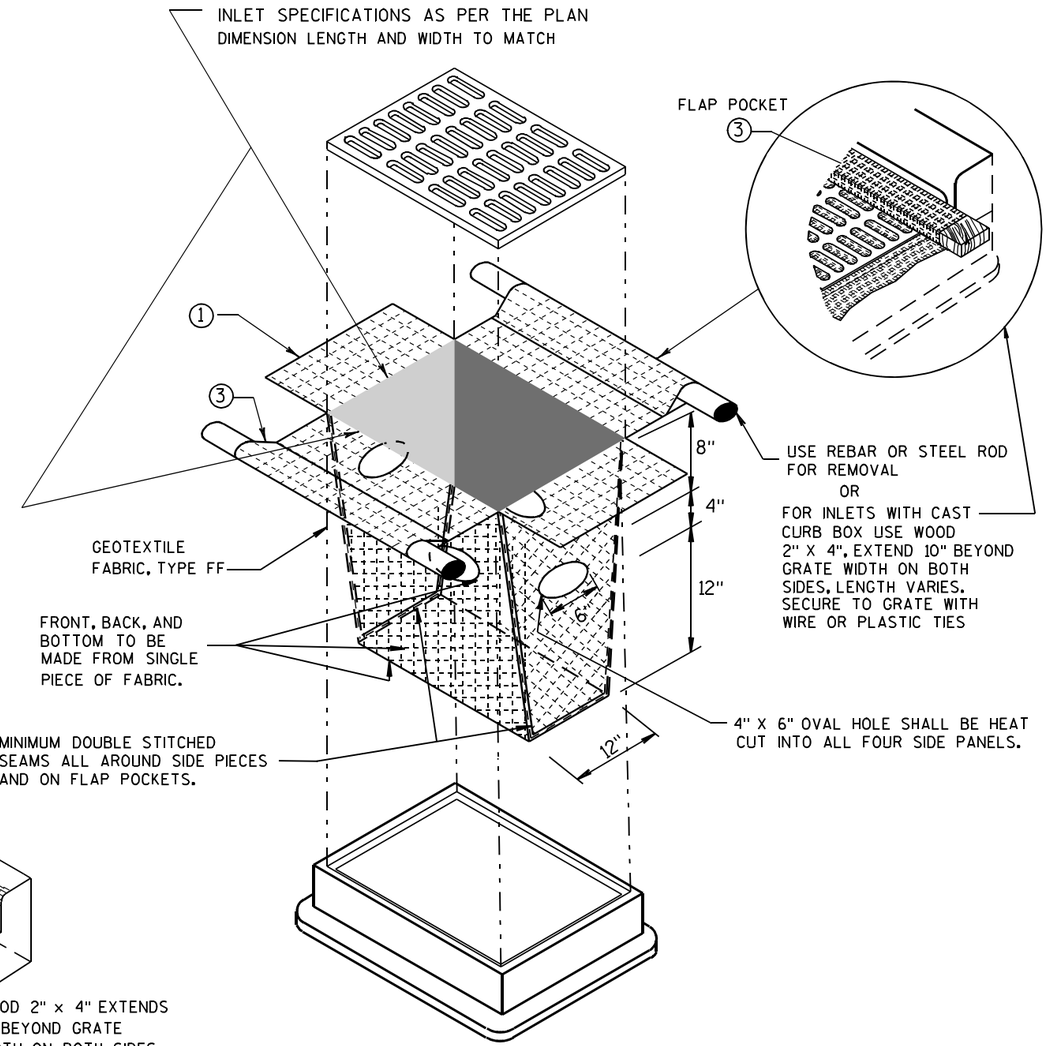


INLET PROTECTION, TYPE A



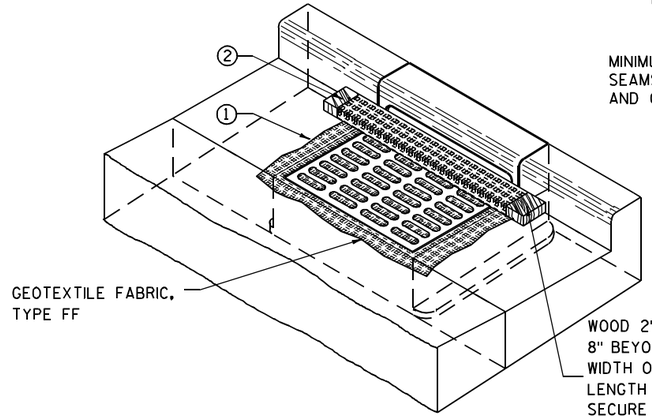
INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE D

(CAN BE INSTALLED IN ANY INLET TYPE WITH
OR WITHOUT A CURB BOX AS PER NOTE ②)



INLET PROTECTION, TYPE C (WITH CURB BOX)

GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.

② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.

③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.

INLET PROTECTION
TYPE A, B, C, AND D

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
10/16/02
DATE
/S/ Beth Cannestra
CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA

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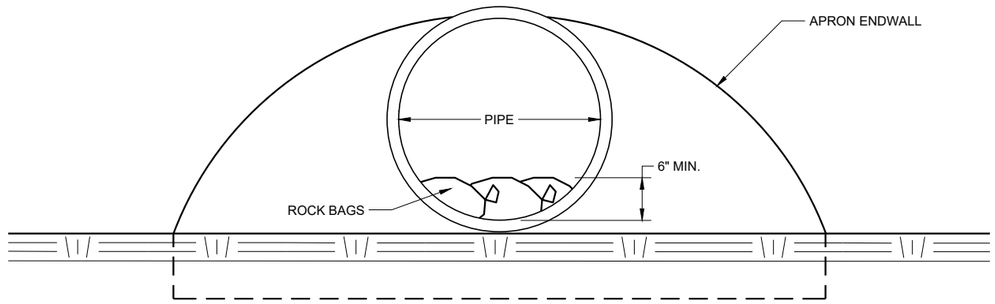
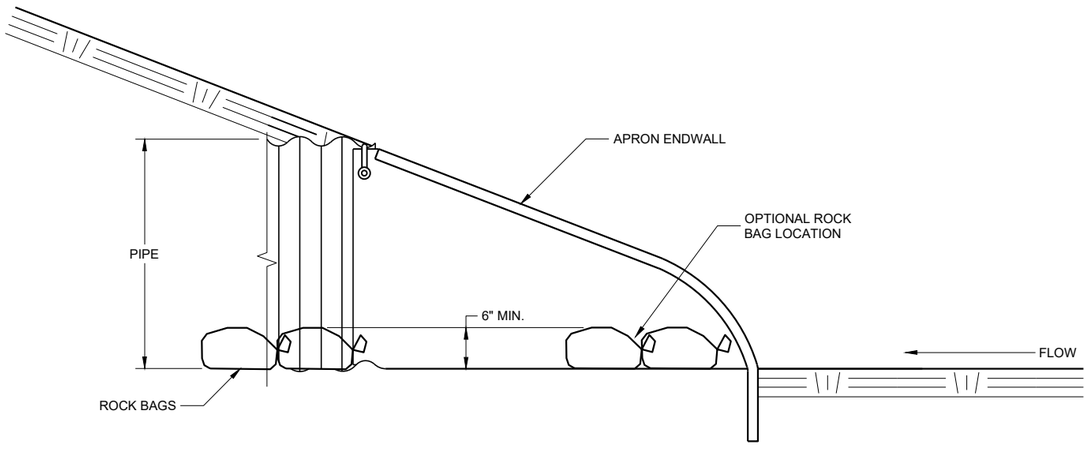
S.D.D. 8 E 10-2



END VIEW



SIDE VIEW

CULVERT PIPE CHECK

(INSTALL ON INLET END ONLY)

CULVERT PIPE CHECK

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2019
DATE

/S/ Daniel Schave
EROSION CONTROL ENGI

FHWA

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SDD 08E15 - 01

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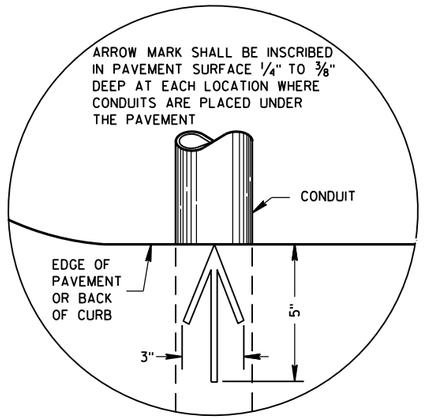
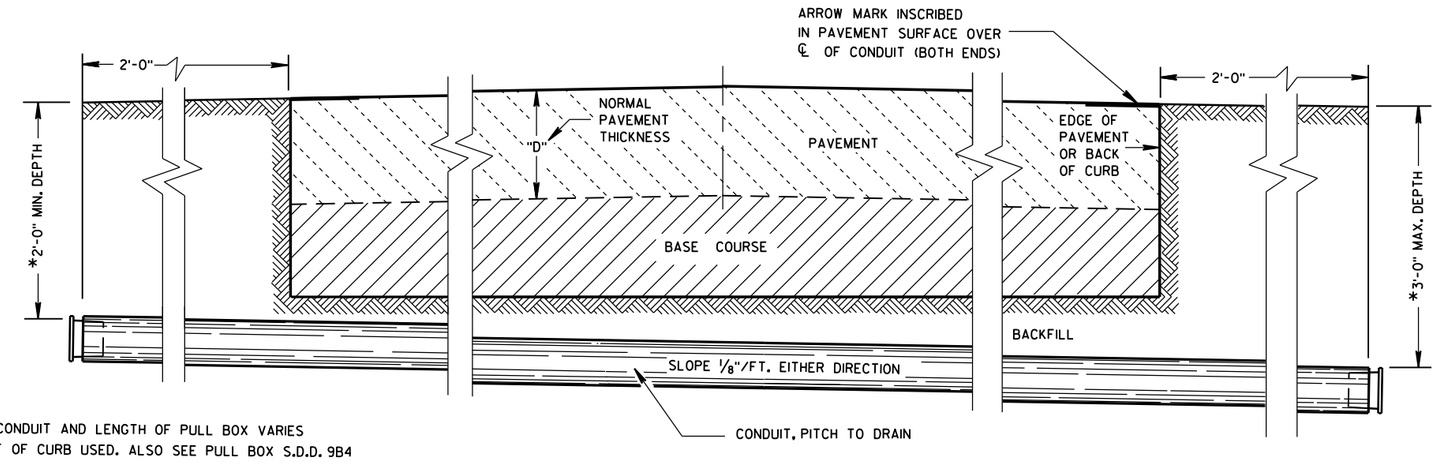
SDD 08E15 - 01



PLAN VIEW

ARROW MARK



*DEPTH OF CONDUIT AND LENGTH OF PULL BOX VARIES WITH HEIGHT OF CURB USED. ALSO SEE PULL BOX S.D.D. 9B4

SIDE ELEVATION

DETAIL FOR CONDUIT UNDER PAVED HIGHWAYS

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

METALLIC (STANDARD SPECIFICATION 652.2.2) OR NONMETALLIC (STANDARD SPECIFICATION 652.2.3) CONDUIT SHALL BE FURNISHED AND PLACED AS SHOWN.

DEPTH OF CONDUIT INSTALLED BELOW THE TRAVELED WAY SHALL BE 24 INCHES MINIMUM AND 36 INCHES MAXIMUM.

DEPTH OF CONDUIT INSTALLED THAT IS NOT BELOW THE TRAVELED WAY SHALL BE 18 INCHES MINIMUM AND 36 INCHES MAXIMUM.

ANY EXCEPTION TO THE MAXIMUM DEPTH SHALL BE ONLY WITH THE WRITTEN APPROVAL OF THE ENGINEER.

THE TRENCH SHALL NOT BE BACKFILLED PRIOR TO INSPECTION OF THE CONDUIT.

ALL METALLIC CONDUIT RACEWAY ENDS SHALL BE REAMED AND THREADED.

ALL METALLIC CONDUIT IN WHICH WIRE OR CABLE IS TO BE INSTALLED SHALL BE BUSHED WITH APPROVED THREADED BUSHINGS BEFORE INSTALLATION OF THE WIRE OR CABLE.

ALL METALLIC CONDUITS IN WHICH WIRE OR CABLE IS NOT TO BE INSTALLED SHALL BE CAPPED WITH THREADED PROTECTIVE CAPS, AS APPROVED BY THE ENGINEER.

ALL NONMETALLIC CONDUIT SHALL BE CAPPED OR PLUGGED IMMEDIATELY AFTER INSTALLATION AND SHALL REMAIN CAPPED OR PLUGGED UNTIL WIRE/CABLES ARE INSTALLED.

NONMETALLIC CONDUITS IN WHICH WIRE OR CABLE IS NOT BEING INSTALLED SHALL REMAIN CAPPED OR PLUGGED.

BENDING OF PVC ELECTRICAL CONDUIT SHALL BE ACCOMPLISHED BY USING A BLANKET OR EMERSION TYPE TANK DESIGNED FOR THE PURPOSE OF BENDING PVC ELECTRICAL CONDUIT.

ALL CUT ENDS SHALL BE TRIMMED INSIDE AND OUTSIDE TO REMOVE ALL ROUGH EDGES ON NONMETALLIC CONDUIT. (SEE NEC 347.5)

WHEN REQUIRED TO CONNECT NONMETALLIC CONDUIT TO METALLIC CONDUIT, ONLY U.L. LISTED ADAPTER FITTINGS SHALL BE USED.

PRIOR TO CONDUIT ACCEPTANCE, CONDUIT CAPS OR PLUGS SHALL BE REMOVED, AND THE CAPS, PLUGS AND CONDUIT ENDS SHALL BE THOROUGHLY CLEANED AND THEN THE CAPS OR PLUGS REINSTALLED TO ENSURE THAT THE CAPS OR PLUGS CAN BE EASILY REMOVED IN THE FUTURE.

ALL CONDUIT BEING FURNISHED AND INSTALLED SHALL HAVE THE U.L. LABEL FIRMLY ATTACHED.

CONDUIT RUNS SHALL BE THE SAME SIZE OF CONDUIT FROM ONE END TO THE OTHER (FROM PULL BOX TO PULL BOX-OR-JUNCTION BOX TO JUNCTION BOX-OR-BASE TO BASE, ETC.).

TRACER WIRE SHALL BE INSTALLED AS STATED IN THE STANDARD SPECIFICATION, ITEM 652.3.1.1.

ALL CONDUIT RUNS SHALL BE STRAIGHT (WITHOUT BENDS) FROM PULL BOX TO PULL BOX, PULL BOX TO BASE AND BASE TO BASE AS SHOWN ON THE PLANS.

CONDUIT

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
March, 2017 — DATE
/S/ Ahmet Demirbilek — STATE ELECTRICAL
FHWA

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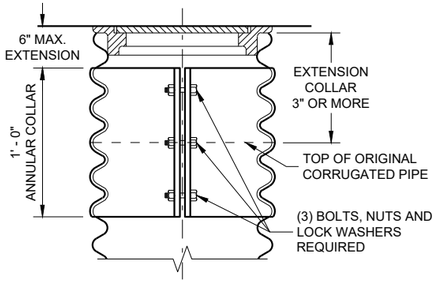
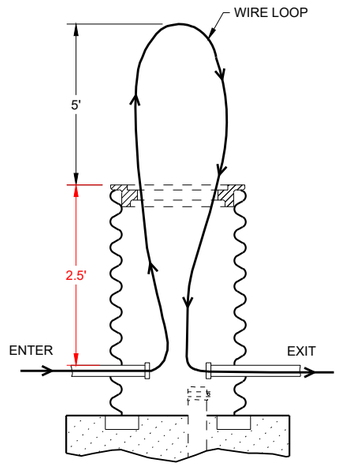
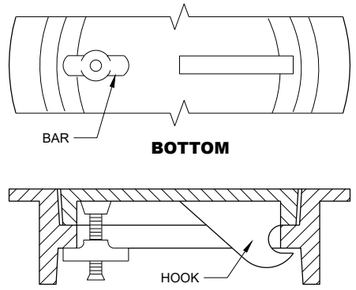


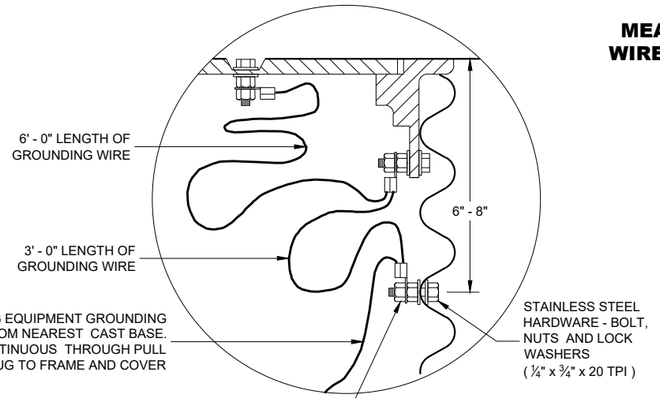
CORRUGATED PIPE EXTENDER



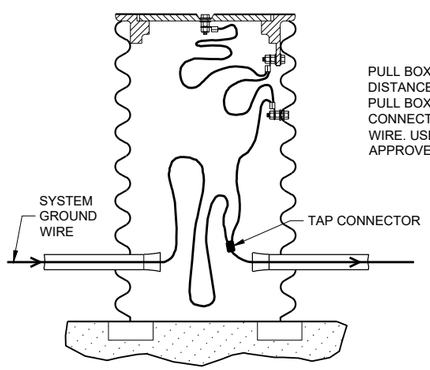
MEASUREMENT DETAIL FOR WIRE/CABLE IN THE PULL BOX



ALTERNATE COVER (LOCKING)
TIGHTENING BAR TYPE

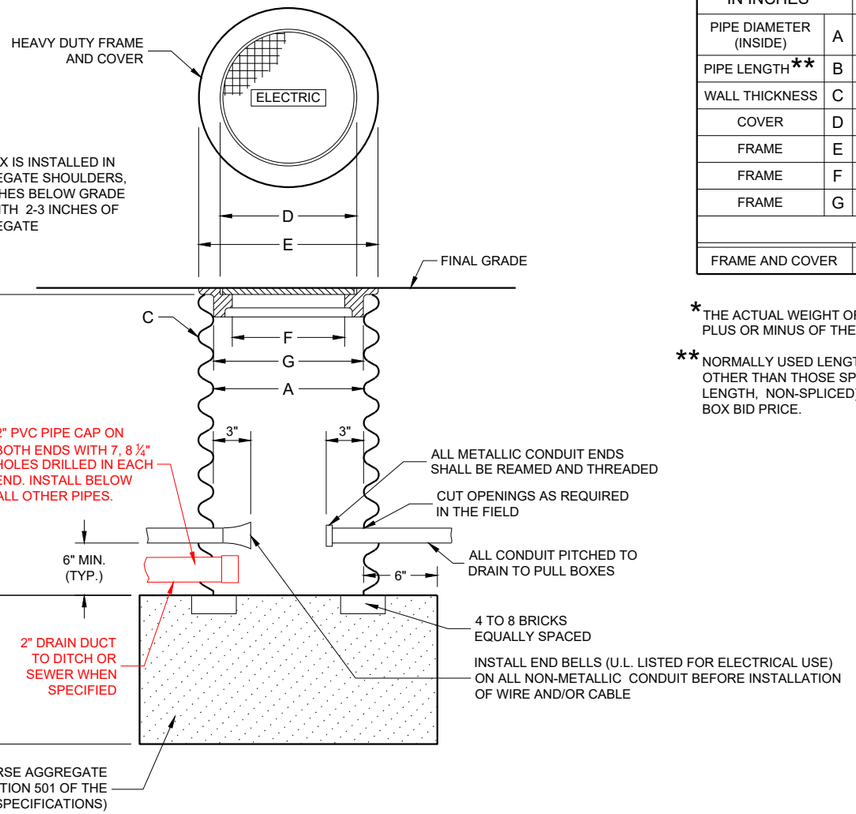


EQUIPMENT GROUNDING LUG AND LOCATION IN STEEL PULL BOXES



PULL BOX TO NEAREST BASE DISTANCE MORE THAN 20 FEET. PULL BOX GROUND WIRE SHALL CONNECT AT SYSTEM GROUNDING WIRE. USE DEPARTMENT APPROVED TAP CONNECTOR.

EQUIPMENT GROUNDING LUG AND LOCATION IN STEEL PULL BOXES



PULL BOX

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

ALL FRAMES AND COVERS SHALL BE HEAVY DUTY TYPE, SUITABLE FOR VEHICULAR TRAFFIC LOADS.

PULL BOXES LOCATED IN THE ROADWAYS SHALL HAVE LOCKING COVERS.

ENTRANCE HOLES INTO PULL BOXES SHALL BE CUT WITH A CIRCULAR HOLE SAW OR HYDRAULIC CONDUIT PUNCH. HOLE SIZE SHALL BE THE OUTSIDE DIAMETER OF THE CONDUIT THAT IS TO FIT IN THE OPENING PLUS NO MORE THAN ¼".

THE CONTRACTOR SHALL NOT INSTALL WIRE IN ANY PULL BOX UNTIL ITS INSTALLATION HAS BEEN INSPECTED AND ACCEPTED BY THE ENGINEER.

GROUNDING LUGS (MECHANICAL CONNECTORS) SHALL BE U.L. LISTED AND APPROVED FOR USE WITH COPPER WIRE.

ALL METALLIC CONDUIT IN WHICH WIRE AND/OR CABLE IS TO BE INSTALLED, SHALL BE BUSHED BEFORE INSTALLATION OF THE WIRE AND/OR CABLE.

WHEN PULL BOXES ARE INSTALLED FOR FUTURE USE, DO NOT INSTALL THE EQUIPMENT GROUNDING LUG. THE EQUIPMENT GROUNDING LUG, THE EQUIPMENT GROUNDING ELECTRODE AND THE EQUIPMENT GROUNDING CONDUCTOR SHALL BE REQUIRED AND INSTALLED UNDER A FUTURE WIRING CONTRACT.

TABLE OF NOMINAL DIMENSIONS AND WEIGHTS

DIMENSION IN INCHES		CORRUGATED STEEL PIPE								
PIPE DIAMETER (INSIDE)	A	12	12	12	18	18	18	24	24	24
PIPE LENGTH**	B	24	30	36	24	30	36	36	42	48
WALL THICKNESS	C	0.064	0.064	0.064	0.064	0.064	0.064	0.064	0.064	0.064
COVER	D	10 ¼	10 ¼	10 ¼	16 ¼	16 ¼	16 ¼	22 ¼	22 ¼	22 ¼
FRAME	E	14 ½	14 ½	14 ½	20 ½	20 ½	20 ½	26 ½	26 ½	26 ½
FRAME	F	8 ½	8 ½	8 ½	14 ½	14 ½	14 ½	20 ½	20 ½	20 ½
FRAME	G	11 ½	11 ½	11 ½	17 ½	17 ½	17 ½	23 ½	23 ½	23 ½
WEIGHT IN POUNDS*										
FRAME AND COVER		60	60	60	110	110	110	155	155	155

*THE ACTUAL WEIGHT OF THE MANHOLE FRAME AND COVER MAY VARY WITHIN 5 PERCENT PLUS OR MINUS OF THE WEIGHTS SHOWN.

**NORMALLY USED LENGTHS. THE PROJECT ENGINEER SHALL DETERMINE IF PIPE LENGTHS, OTHER THAN THOSE SPECIFIED, SHALL BE USED, TO A MAXIMUM OF 48" (CONTINUOUS LENGTH, NON-SPLICED). THE ADDITIONAL LENGTH SHALL BE INCIDENTAL TO THE PULL BOX BID PRICE.

PULL BOX

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2022
DATE

/S/ Ahmet Demirbilek
STATE ELECTRICAL ENGI

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FHWA

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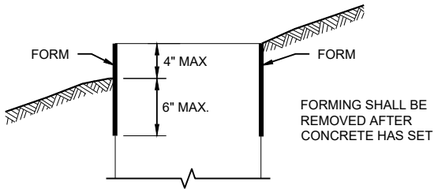
SDD 09B04-12

FORM DEPTH SHALL BE NO MORE THAN 6" BELOW GRADE ON THE LOWER SIDE OF BASE



FORMING DETAIL

QUANTITY REQUIREMENTS	CONCRETE BASE TYPE		
	1	2	5 & 6
APPROX. CUBIC YARDS OF CONCRETE	0.40	0.57	0.40
LBS. OF HOOP BAR STEEL	NONE	23	16
LBS. OF VERTICAL BAR STEEL	NONE	60	18

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWINGSHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

BASES SHALL BE EXCAVATED BY USE OF A CIRCULAR AUGER.

TOP SURFACES OF CONCRETE BASES SHALL BE TROWEL FINISHED SMOOTH AND LEVEL.

CONDUIT SIZES AND LOCATIONS SHALL BE SHOWN ON THE PLANS

THE FINAL OR TERMINATING CONCRETE BASE IN A CONDUIT RUN SHALL HAVE A 6" EXIT STUB INSTALLED FOR FUTURE CABLING USE. THE EXIT STUB SHALL BE SIZED AS USED THROUGHOUT THE CONDUIT RUN AS SHOWN A THE ENTRANCE OF THE BASE.

ENDS OF CONDUIT INSTALLED BELOW GRADE FRO FUTURE USE SHALL BE CAPPED IF METALLIC OR PLUGGED IF NON-METALLIC.

MINIMUM BENDING RADIUS OF CONDUIT IS EQUAL TO 6X THE DIAMETER.

CONDUIT HEIGHT ABOVE CONCRETE BASES SHALL BE 1 INCH. ALL METALLIC CONDUIT ENDS SHALL BE REAMED AND THREADED.

ALL CONDUIT ENDS AT THE TOP OF CONCRETE BASES SHALL BE CAPPED IF METALLIC OR PLUGGED IF NON-METALLIC IMMEDIATELY AFTER PLACEMENT AND BEFORE CONCRETE IS POURED. CONDUITS IN WHICH WIRE OR CABLE IS NOT INSTALLED SHALL REMAIN CAPPED OR PLUGGED.

BELL ENDS SHALL BE INSTALLED ON ALL PVC CONDUIT EXPOSED AT THE TOP OF CONCRETE BASES BEFORE INSTALLATION.

WHEN REQUIRED TO CONNECT NON-METALLIC CONDUIT TO METALLIC CONDUIT, ONLY ADAPTER FITTINGS, U.L. LISTED FOR ELECTRICAL USE, SHALL BE USED.

IF A BASE REQUIRES A DEEP FORM BECAUSE OF LOOSE DIRT OR FILL, THE FORM SHALL BE REMOVED BEFORE BACKFILLING AROUND THE BASE. BACKFILL SHALL BE TAMPED TIGHT AGAINST THE BARE CONCRETE BASE IN LAYERS OF 1 FOOT OR LESS.

A NO. 4 AWG STRANDED COPPER EQUIPMENT GROUNDING CONDUCTOR SHALL BE EXOTHERMICALLY WELDED TO THE EQUIPMENT GROUNDING ELECTRODE (GROUND ROD) FOR TYPE 2, TYPE 5 AND TYPE 6 BASES.

THE EQUIPMENT GROUNDING CONDUCTOR SHALL BE FURNISHED AND INSTALLED TO ENTER ALL BASE TYPES THROUGH A 1 INCH CONDUIT INSTALLED FOR GROUNDING PURPOSES, LEAVING A 4 FOOT COIL OF WIRE ABOVE THE CONCRETE BASE. THE EQUIPMENT GROUNDING CONDUCTOR SHALL BE NEATLY COILED AND THE COILS TIED TOGETHER.

ANCHOR RODS SHALL BE THREADED 12" IN LENGTH ON EACH END OF THE ROD. ANCHOR RODS SHALL BE MANUFACTURED IN ACCORDANCE WITH SECTION 654.2.1 OF THE STANDARD SPECIFICATIONS.

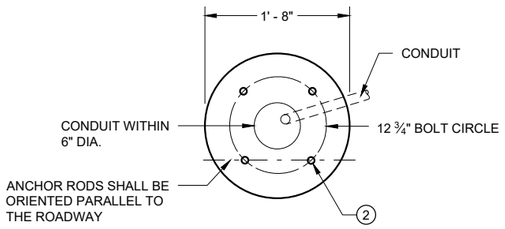
WASHERS AND LOCK WASHERS ARE REQUIRED ON ALL ANCHOR RODS.

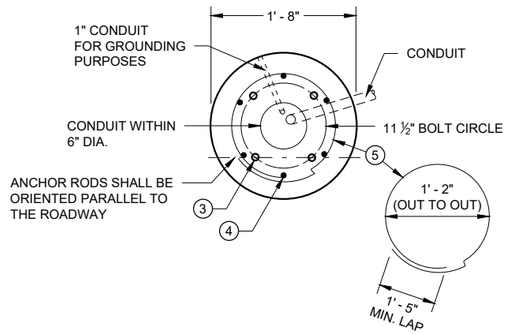
WHEN ANCHOR RODS USING THE ALTERNATE "L" BEND ARE FURNISHED, THE 4 INCH"L" BEND SHALL BE IN ADDITION TO THE SPECIFIED ANCHOR ROD BAR LENGTH. THE "L" BEND SHALL NOT BE THREADED.

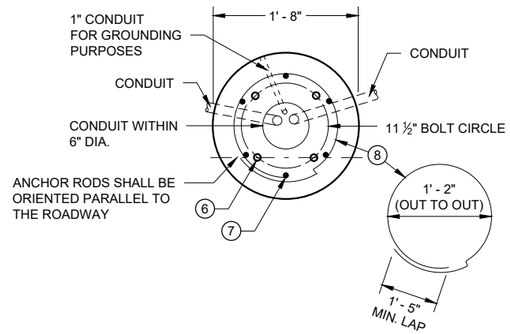
ANCHOR RODS SHALL BE INSTALLED WITH MISALIGNMENTS OF LESS THAN 1:40 FROM VERTICAL.

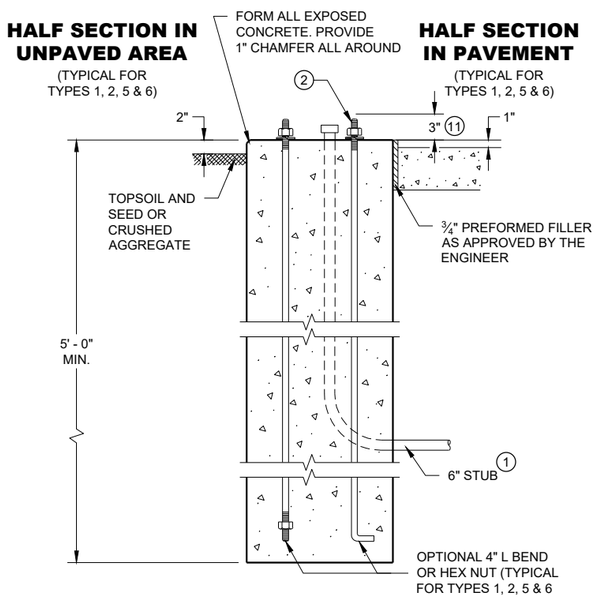
WELDING OF THE ANCHOR RODS TO THE CAGE IS UNACCEPTABLE. TIE WIRES SHALL BE USED.

BAR STEEL REINFORCEMENT SHALL BE COATED WITH POWDERED EPOXY RESIN IN ACCORDANCE WITH SECTION 505 OF THE STANDARD SPECIFICATIONS (LATEST EDITION).

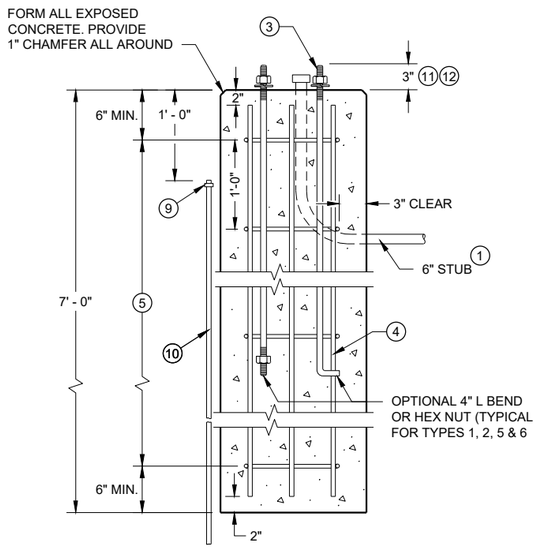




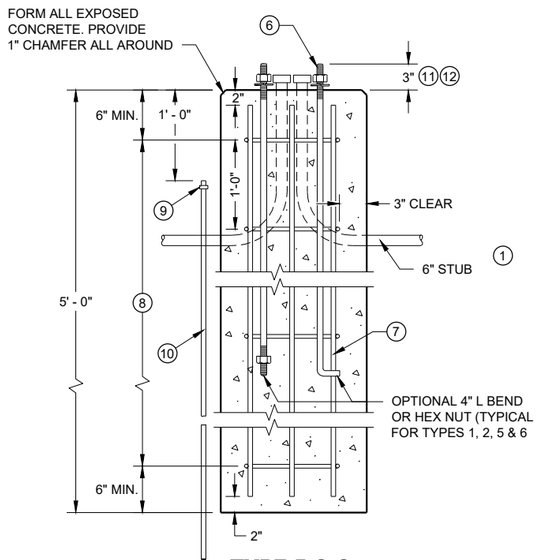




TYPE 1



TYPE 2



TYPE 5 & 6

(1) THE MINIMUM DEPTH OF CONDUIT EXITING THE CONCRETE BASE AND INSTALLED BELOW THE TRAVELED WAY SHALL BE 24 INCHES. THE MINIMUM DEPTH OF CONDUIT EXITING THE CONCRETE BASE THAT IS NOT INSTALLED BELOW THE TRAVELED WAY SHALL BE 18 INCHES. THE MAXIMUM DEPTH OF ALL CONDUIT SHALL BE 36 INCHES EXCEPT WITH WRITTEN APPROVAL OF THE ENGINEER.

(2) (4) 1" DIA. X 3' - 6" ANCHOR RODS.

(3) (4) 1" DIA. X 5' - 0" ANCHOR RODS.

(4) (6) NO. 6 X 6' - 8" BAR STEEL REINFORCEMENT.

(5) (7) NO. 4 X 5' - 1" BAR STEEL REINFORCEMENT @ 1' - 0" C - C.

(6) (4) 1" DIA. X 3' - 6" ANCHOR RODS.

(7) (6) NO. 4 X 4' - 8" BAR STEEL REINFORCEMENT.

(8) (5) NO. 4 X 5' - 1" BAR STELL REINFORCEMENT @ 1' - 0" C -C.

(9) EXOTHERMIC CONNECTION TO EUIPMENT GROUNDING CONDUCTOR

(10) ⅝" DIA. X 8' -0" COPPERCLAD EQUIPMENT GROUNDING ELECTRODE REQUIRED

(11) ANY ANCHOR ROD PROJECTION SHORTER THAN 2 ¾" OR LONGER THAN 3 ¼" SHALL REQUIRE THE BASE TO BE REMOVED AND REPLACED AT THE CONTRACTORS EXPENSE.

(12) FOR NON - BREAKAWAY INSTALLATIONS, 4 ½" ± ANCHOR ROD PROJECTION WITH THE USE OF LEVELING NUTS. RODENT SCREEN REQUIRED.

CONCRETE BASES

CONCRETE BASES TYPES 1, 2, 5, & 6

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2019
DATE
/S/ Ahmet Demirbilek
STATE ELECTRICAL ENGI

FHWA

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SDD 09C02 - 09

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

FOUR (4) BOLTS SHALL BE FURNISHED WITH EACH TRANSFORMER BASE. BOLTS SHALL BE 1" DIAMETER, 4" IN LENGTH, WITH WASHERS, LOCK WASHERS AND NUTS. BOLTS, NUTS AND WASHERS SHALL BE MANUFACTURED IN ACCORDANCE WITH SECTION 531.2.2 OF THE STANDARD SPECIFICATIONS.

LEVELING SHIMS, IF NEEDED, SHALL BE DESIGNED FOR THE PURPOSE AND USED UNDER CAST BASES WHEN PLUMBING POLES OR STANDARDS DURING INSTALLATION. THE USE OF WASHERS IN LIEU OF PROPER LEVELING SHIMS IS NOT ACCEPTABLE.

SHIM LENGTH SHALL BE LONG ENOUGH TO COMPLETELY COVER THE AREA UNDER THE LENGTH AND WIDTH OF THE BASE MOUNTING FLANGE.

DOUBLE NUTTING IS NOT ACCEPTABLE FOR LEVELING OR MOUNTING PURPOSES.

A NEMA APPROVED, U.L. LISTED, COPPER WITH BRASS OR STAINLESS STEEL SET SCREW, DIRECT BURY RATED, MECHANICAL CONNECTOR (LUG), SIZED TO ACCEPT AWG. #10 TO #4 COPPER STRANDED WIRE SHALL BE FURNISHED AND INSTALLED IN THE PEDESTAL AND TRANSFORMER BASES.

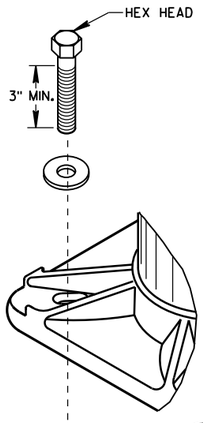
THE MECHANICAL CONNECTOR SHALL BE INSTALLED USING A 1/4" - 20 (TPI) STAINLESS STEEL HEX HEAD BOLT OF SUFFICIENT LENGTH TO FIRMLY ATTACH THE LUG TO THE BASE.

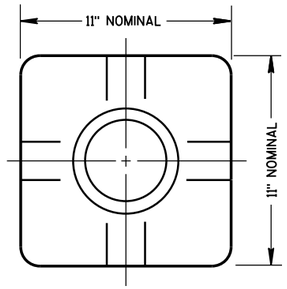
SHOULD THE MANNER OF ATTACHMENT OF THE LUG REQUIRE WASHERS, HEX NUTS, LOCK WASHER - THEY SHALL BE STAINLESS STEEL AS IS THE BOLT. THE MANNER OF ATTACHMENT SHALL NOT BLOCK ACCESSIBILITY TO WIRE PLACEMENT IN THE CONNECTOR.

PEDESTAL BASE COLLAR THREADING SHALL BE TAPERED AND IN ACCORDANCE WITH NATIONAL PIPE THREADING DIMENSIONS.

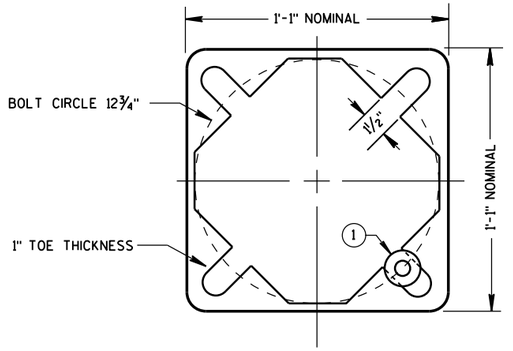
BASE COLLAR THREADING SHALL EXTEND INTO THE BASE COLLAR WITH SUFFICIENT DEPTH TO ACCEPT THE INSTALLATION OF TRAFFIC SIGNAL STANDARDS TO A DEPTH OF 1 1/2", THEN TIGHTENING TO A POINT OF BEING IMMOVABLE.

THE ACCESS DOOR SHALL BE OF THE SAME MATERIAL AS THE BASE.

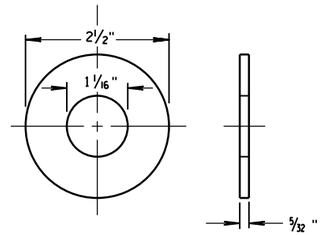




TOP VIEW
(PEDESTAL BASE)

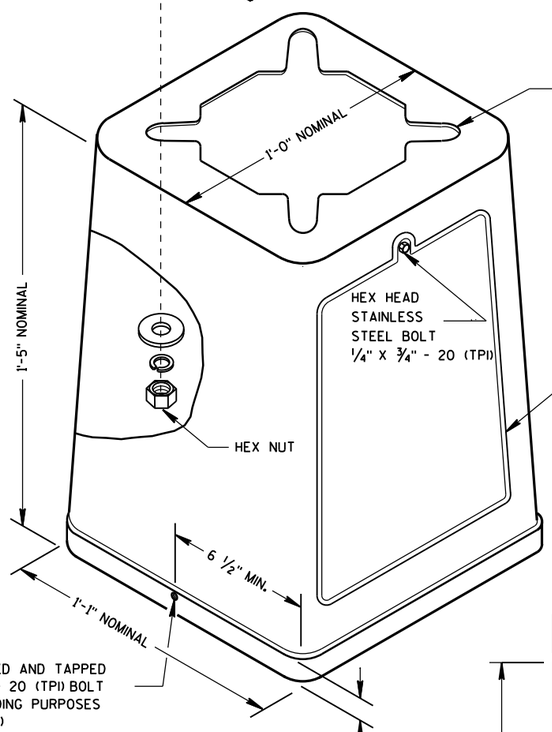


BOTTOM VIEW
(PEDESTAL BASE)

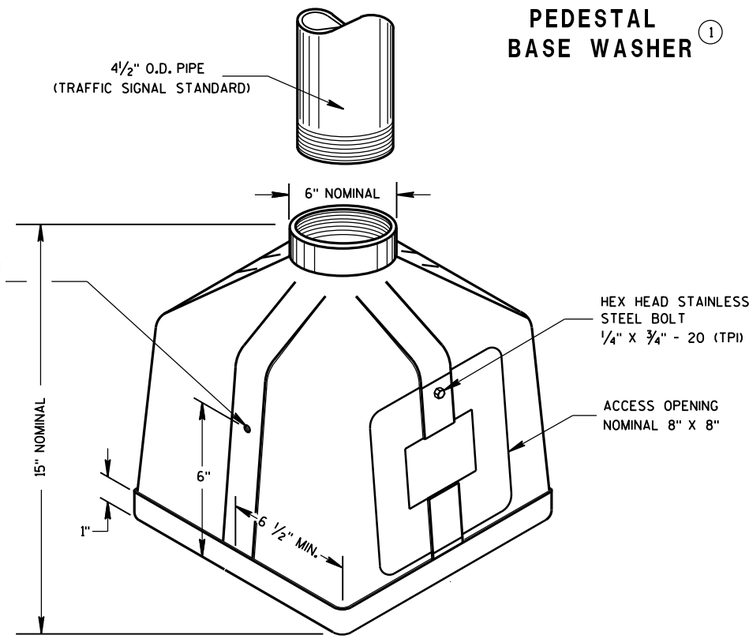


ZINC COATED STEEL WASHER
TO BE PROVIDED BY THE CONTRACTOR

PEDESTAL BASE WASHER (1)

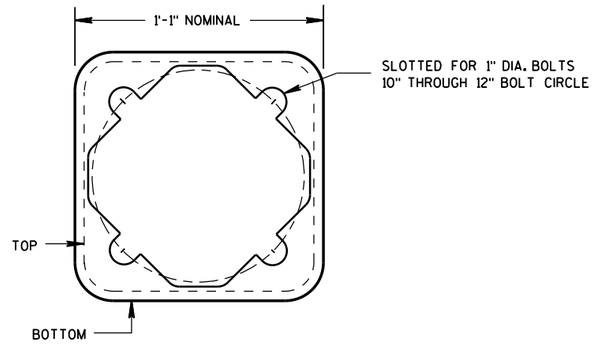


ISOMETRIC VIEW

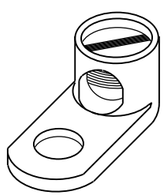


ISOMETRIC VIEW

PEDESTAL BASE



BOTTOM VIEW
(TRANSFORMER BASE)

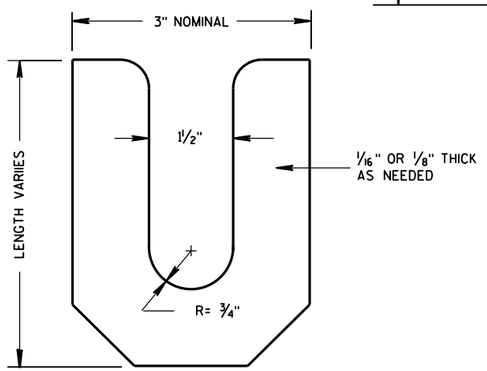


TYPICAL MECHANICAL CONNECTOR LUG

TO BE FURNISHED WITH EACH BASE

TRANSFORMER BASE

INTENDED FOR USE WITH TYPE 2, 3, 4, 5 & 6 POLES



LEVELING SHIM

TRANSFORMER/PEDESTAL BASES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

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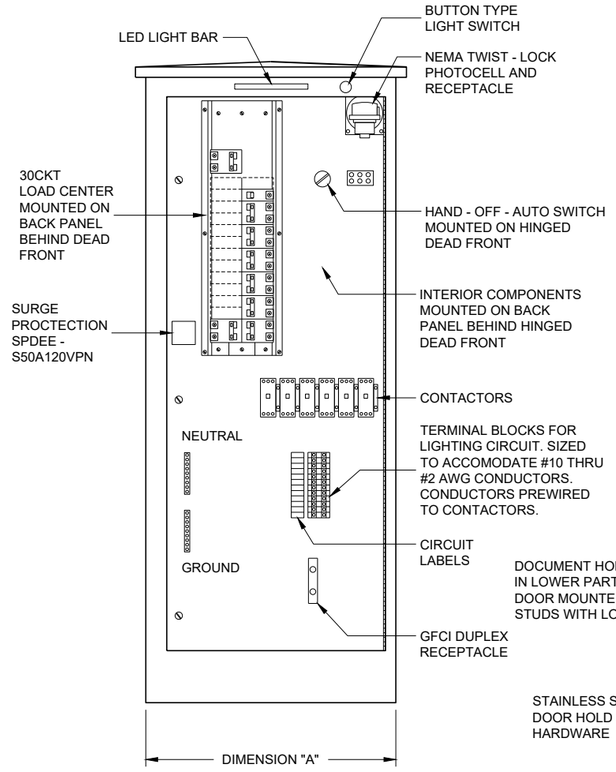
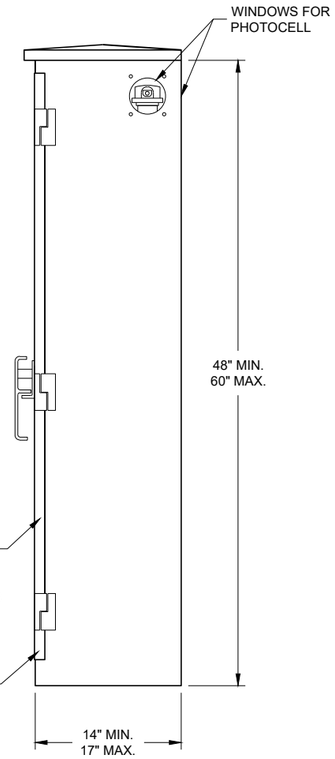
FRONT INTERIOR ELEVATION



SIDE VIEW



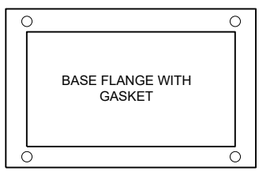
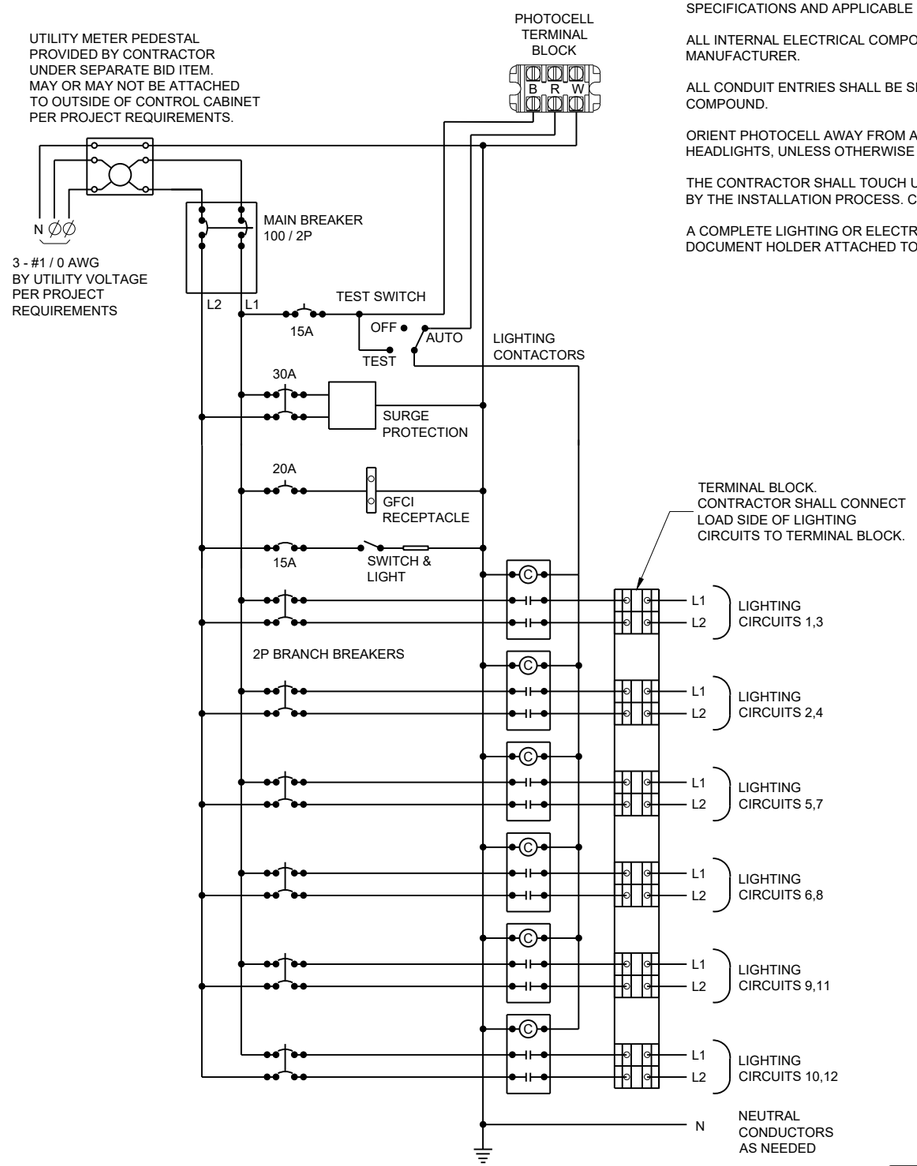


TABLE OF DIMENSIONS (INCHES)		
CONCRETE BASE TYPE	CABINET WIDTH	DIMENSION "A"
L24	24"	24"
L30	30"	30"

LIGHTING CONTROL CABINET



CONTROL CABINET SCHEMATIC

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

ALL INTERNAL ELECTRICAL COMPONENTS WILL BE PRE - WIRED BY THE CABINENT MANUFACTURER.

ALL CONDUIT ENTRIES SHALL BE SEALED WITH AN APPROPRIATE DUCT SEALING COMPOUND.

ORIENT PHOTOCELL AWAY FROM AMBIENT LIGHT SOURCES AND ONCOMING TRAFFIC HEADLIGHTS, UNLESS OTHERWISE CALLED FOR IN THE SPECIAL PROVISION.

THE CONTRACTOR SHALL TOUCH UP ANY DAMAGE TO THE ANODIZED FINISH CAUSED BY THE INSTALLATION PROCESS. COLOR MATCH PAINT SHALL BE USED.

A COMPLETE LIGHTING OR ELECTRICAL PLAN SHALL BE SECURELY PLACED IN THE DOCUMENT HOLDER ATTACHED TO THE DOOR.

LIGHTING CONTROL CABINET
120 / 240 VOLT

STATE OF WISCONSIN
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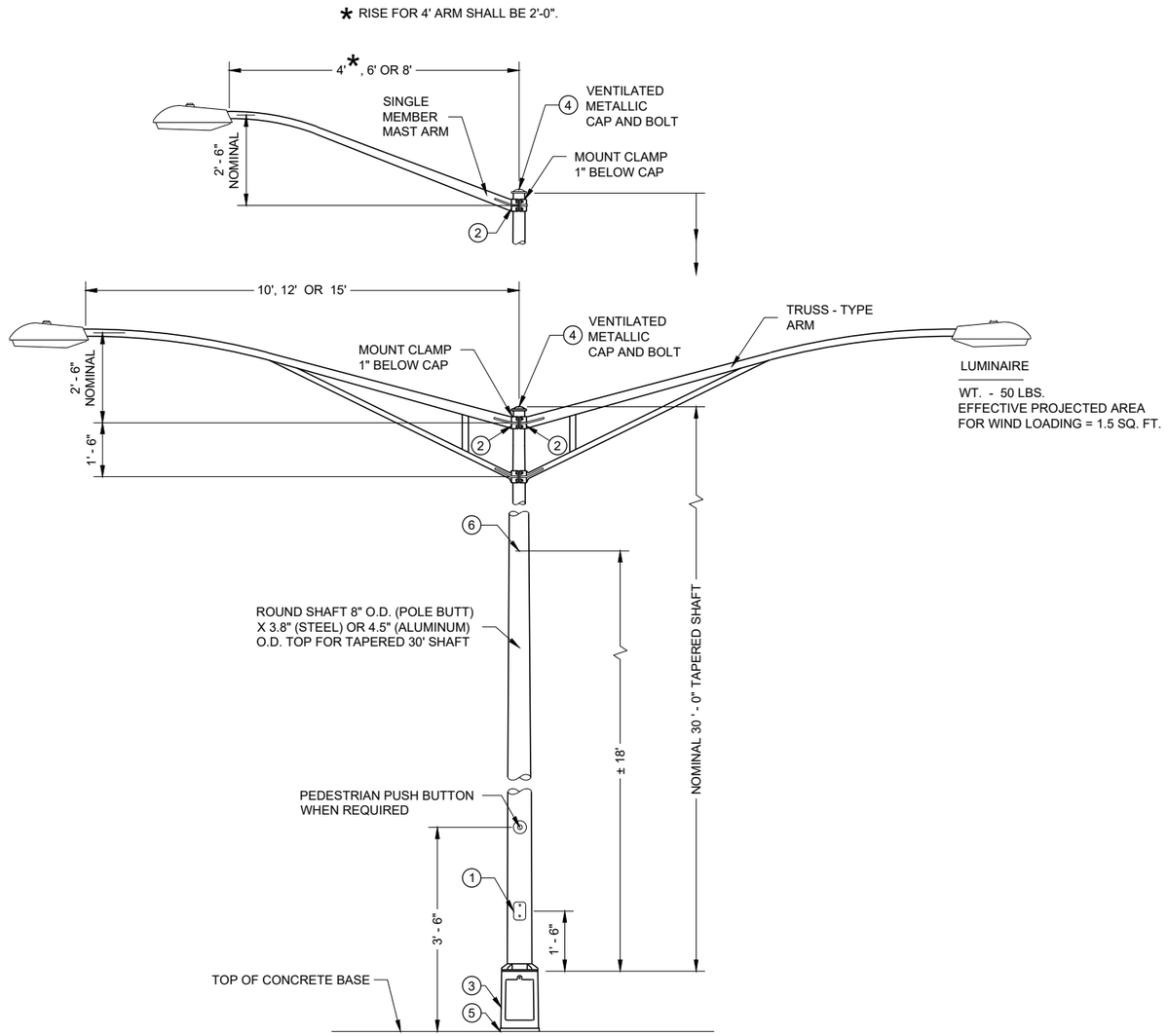


TYPE 5 POLE MOUNTING CONFIGURATION
(MAXIMUM LOAD)
LIGHTING ONLY

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

SECTION 657, POLES, OF THE STANDARD SPECIFICATIONS SHALL APPLY TO THIS DRAWING.

ALL TYPE 5 POLE MOUNTINGS SHALL BE DESIGNED TO INCLUDE TWIN 15' ARMS WITH LUMINAIRES.

POLES SHALL BE GALVANIZED STEEL OR ALUMINUM, AS CALLED FOR IN THE CONTRACT.

TYPE 5 ALUMINUM POLES SHALL BE CONSTRUCTED OF 6063 - T6 ALUMINUM ALLOY. SLEEVING INSIDE THE POLE IS NOT ACCEPTABLE.

TYPE 5 ALUMINUM POLES SHALL HAVE A MINIMUM WALL THICKNESS OF 0.1888".

TYPE 5 STEEL POLES SHALL HAVE A MINIMUM WALL THICKNESS OF U.S. STANDARD 11 GAGE (0.1196").

THE SLIPFITTER END OF THE LUMINAIRE MAST ARM SHALL BE A NOMINAL 2 ⅜ INCHES IN OUTSIDE DIAMETER. THE STRAIGHT PORTION OF THE SLIPFITTER END OF THE LUMINAIRE MAST ARM SHALL BE A NOMINAL 12 INCHES IN LENGTH.

WHEN TRANSFORMER BASES ARE USED, WIRE CONNECTIONS SHALL BE MADE IN THE TRANSFORMER BASE.

(1) 4" X 6" REINFORCED HANDHOLE AND COVER ASSEMBLY WITH TWO (2) ¼" X ¾" - 20 TPI , STAINLESS STEEL, HEX HEAD BOLTS.

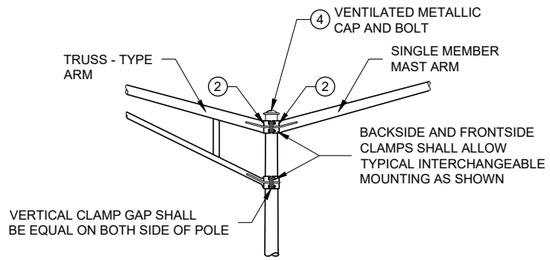
(2) GROMMETS. 1" CHASE NIPPLES OR 1" CLOSE CONDUIT NIPPLES WITH BUSHINGS SHALL BE PROVIDED FOR 1 ⅜" HOLE IN POLE SHAFT FOR WIRING.

(3) CAST ALUMINUM TRANSFORMER BASE, WHEN REQUIRED.

(4) FURNISH AND INSTALL VENTILATED, CAST METALLIC (ALUMINUM ALLOY) CAPS. FASTEN CAPS WITH ONE (1) ¼" X ¾" - 20 TPI STAINLESS STEEL, HEX HEAD BOLT.

(5) SHIMMING, IF NEEDED, SHALL BE LOCATED BETWEEN THE CONCRETE FOUNDATION AND POLE.

(6) INTERNAL DUMBBELL - TYPE VIBRATION DAMPER.



INTERCHANGEABLE MOUNTING DETAIL

POLE MOUNTINGS FOR LIGHTING UNITS, TYPE 5 (30 FEET)

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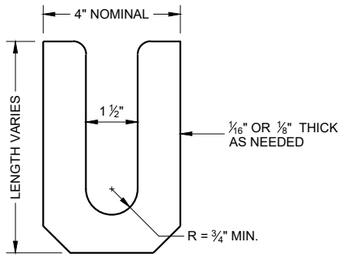
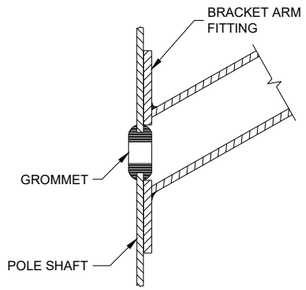
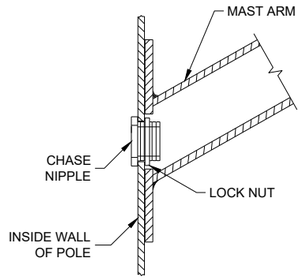


LEVELING SHIM (1)
SHALL BE ALUMINUM



TYPICAL APPLICATION OF GROMMET IN POLE SHAFT



TYPICAL APPLICATION OF CHASE NIPPLE IN POLE SHAFT

GENERAL NOTES

CLAMP BOLT-NUT TIGHTENING TORQUE SHALL BE INDICATED BY INDENT STAMPING (1/2 INCH NUMERALS AND LETTERS) OR WEATHERPROOF PRINTING ON THE INSIDE OF THE CLAMP THAT IS WELDED TO THE ARM MEMBER.

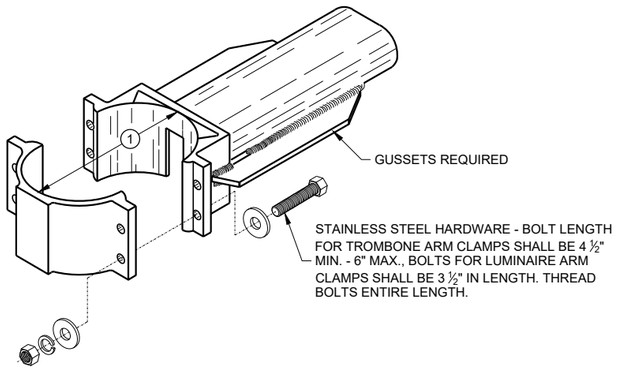
(1) 4.5" I.D. FOR LUMINAIRE MAST ARM CLAMP. 6.625" I.D. FOR TROMBONE MAST ARM CLAMP.

(2) INDIVIDUAL BASE PLATE ANCHOR ROD COVERS. (4 REQUIRED)

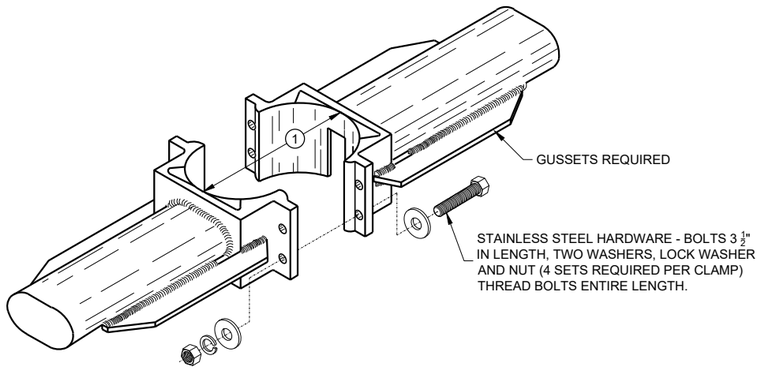
(3) BASE PLATE SLOTTED TO ACCEPT 11" THROUGH 12" BOLT CIRCLE USING 1" DIAMETER ANCHOR RODS.

(4) LEVELING SHIMS, DESIGNED FOR THE PURPOSE, SHALL BE USED WHEN PLUMBING POLES. THE USE OF WASHERS IN LIEU OF PROPER LEVELING SHIMS IS NOT ACCEPTABLE. LEVELING SHIMS SHALL BE USED ONLY BETWEEN THE TOP OF THE CONCRETE BASE AND A METALLIC BASE PLATE.

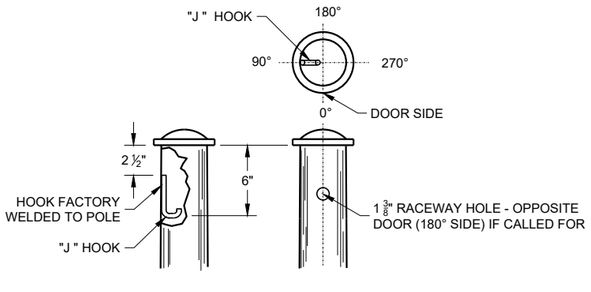
SHIMS SHALL BE LONG ENOUGH AND WIDE ENOUGH TO COMPLETELY COVER THE AREA UNDER THE LENGTH AND WIDTH OF THE BASE MOUNTING FLANGE.



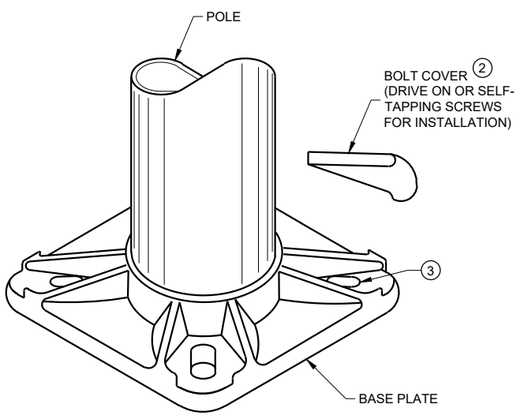
TYPICAL TROMBONE MAST ARM AND SINGLE LUMINAIRE MAST ARM MOUNTING CLAMP



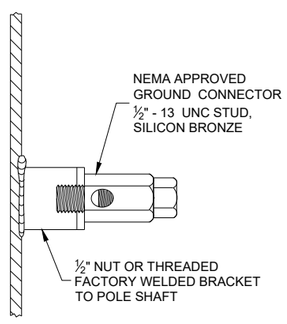
TYPICAL LUMINAIRE MAST ARM (DOUBLE) MOUNTING BRACKETS

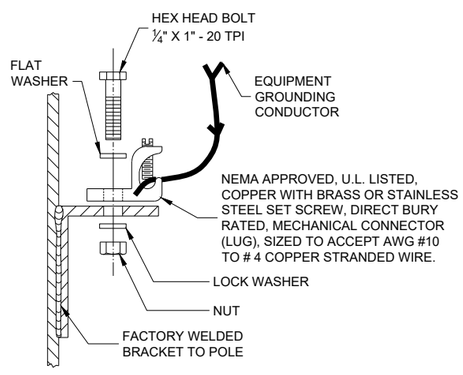


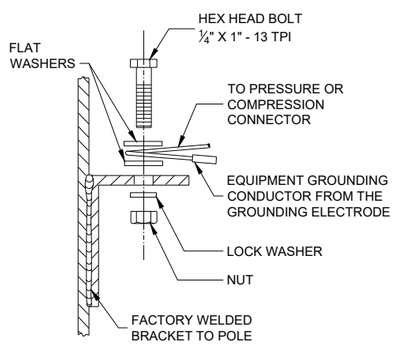
TYPICAL "J" HOOK LOCATION



BASE PLATE







TYPICAL GROUNDING CONNECTIONS
NUT, BOLT AND WASHERS SHALL BE STAINLESS STEEL

HARDWARE DETAILS FOR POLE MOUNTING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2018
DATE

/S/ Ahmet Demirbilek
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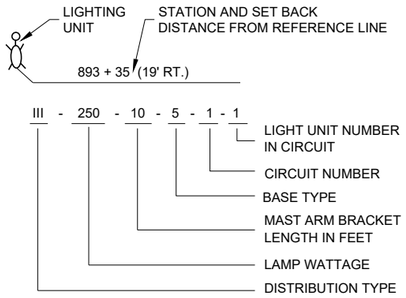
GENERAL NOTES

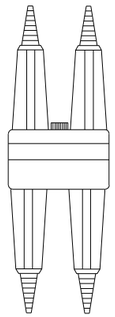
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

THE EQUIPMENT GROUND CONNECTOR SHALL BE TAPED WITH 3 WRAPS (MINIMUM) OF APPROVED RUBBER TAPE AND 3 WRAPS (MINIMUM) OF APPROVED VINYL TAPE TO COVER SHARP WIRE ENDS AFTER THE CONNECTION IS COMPLETED.

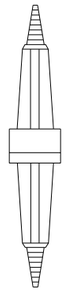
WHEN TRANSFORMER BASES ARE USED, ALL WIRING CONNECTIONS SHALL OCCUR WITHIN THE TRANSFORMER BASES.



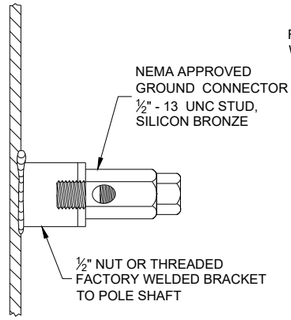
LIGHTING UNIT CODE
(TYPICAL)

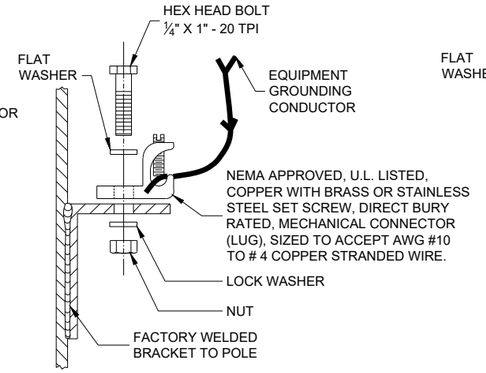


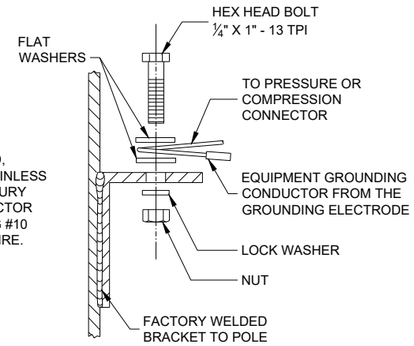
DETAIL "A"
BREAKAWAY DOUBLE POLE WITH WATERPROOF INSULATING BOOT



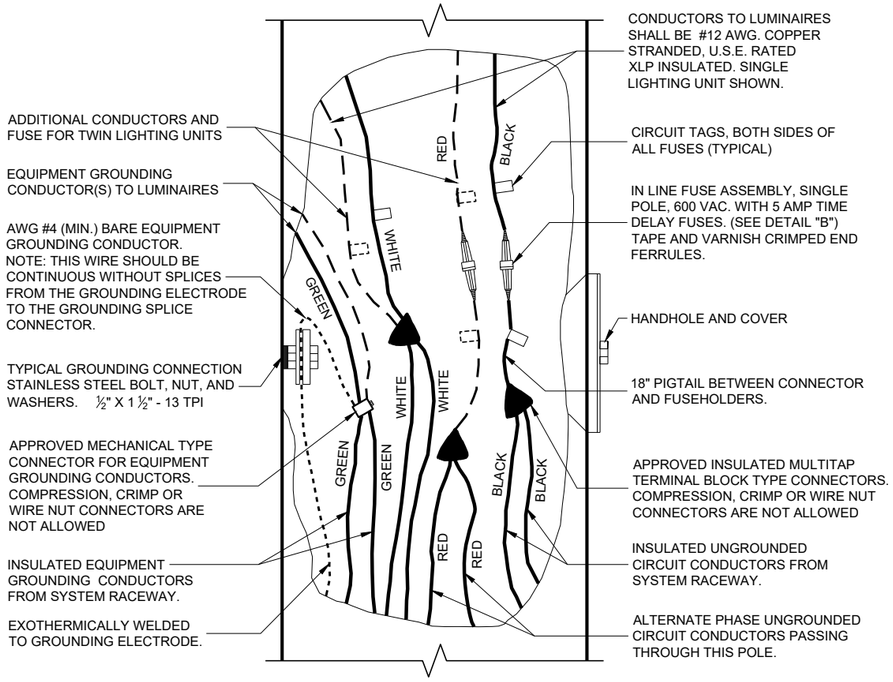
DETAIL "B"
BREAKAWAY SINGLE POLE WITH WATERPROOF INSULATING BOOT



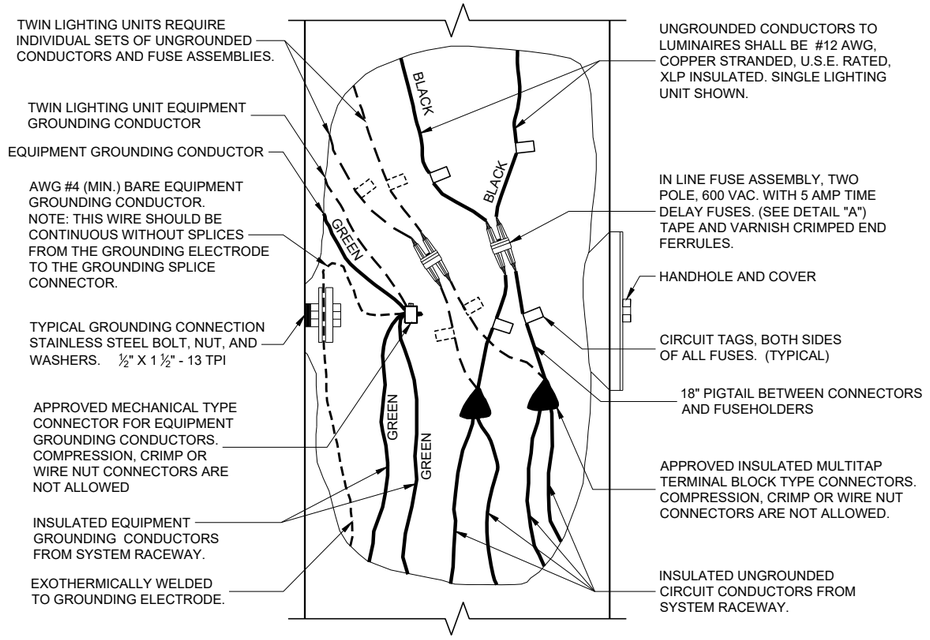




TYPICAL GROUNDING CONNECTIONS
NUT, BOLT AND WASHERS SHALL BE STAINLESS STEEL



3 WIRE - 120, 240 OR 480 VAC (UNGROUNDED CONDUCTORS) WITH GROUNDING CONDUCTOR AND EQUIPMENT GROUNDING CONDUCTOR



2 WIRE - 240 OR 480 VAC (UNGROUNDED CONDUCTORS) WITH EQUIPMENT GROUNDING CONDUCTOR

NON - FREEWAY LIGHTING UNIT POLE WIRING

STATE OF WISCONSIN
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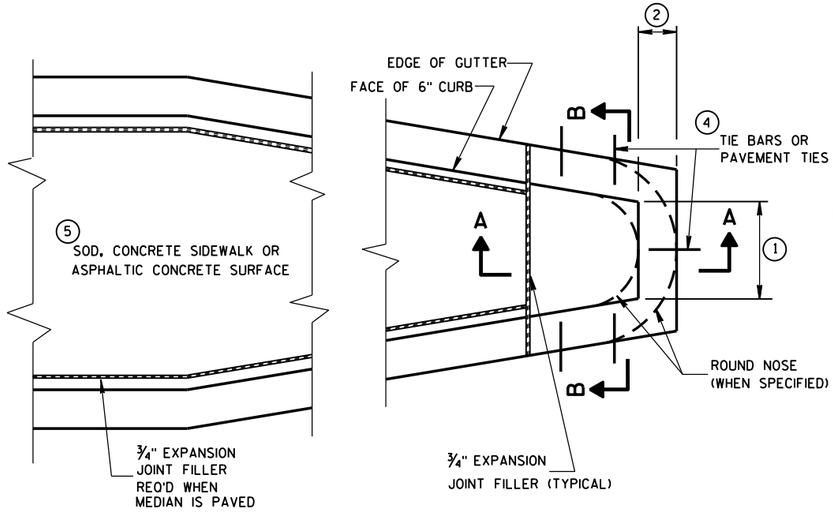
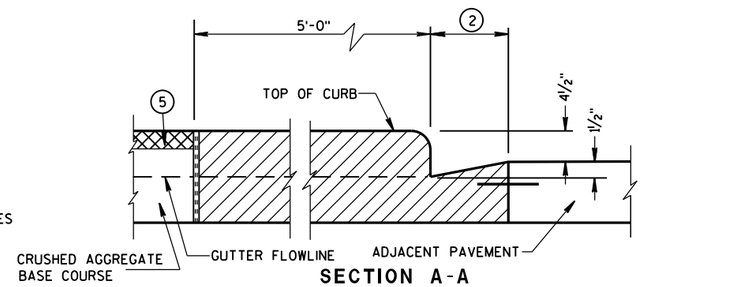
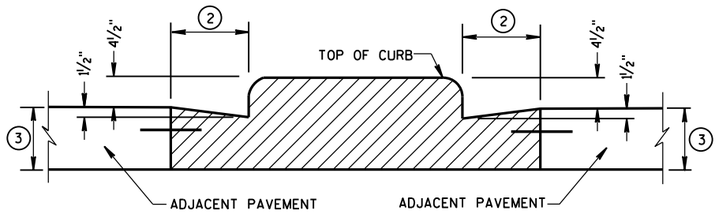






CONCRETE MEDIAN BLUNT NOSE DETAIL

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

(1) SEE PLAN FOR MEDIAN NOSE WIDTH AND RADIUS (FOR ROUND NOSE ALTERNATE).

(2) WIDTH OF GUTTER TO MATCH EXISTING ADJACENT GUTTER OR AS SPECIFIED ELSEWHERE IN THE PLAN.

(3) DEPTH EQUAL TO ADJACENT PAVEMENT. ADJACENT PAVEMENT STRUCTURE DETAILS ARE SHOWN ON THE PLAN. TYPICAL OPTIONS ARE:

(1) NEW OR EXISTING CONCRETE PAVEMENT.

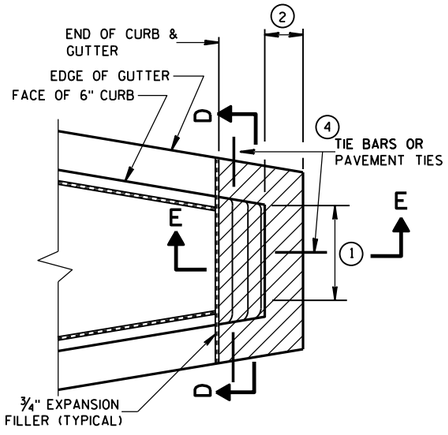
(2) ASPHALTIC CONCRETE PAVEMENT OVER NEW OR EXISTING CONCRETE BASE COURSE.

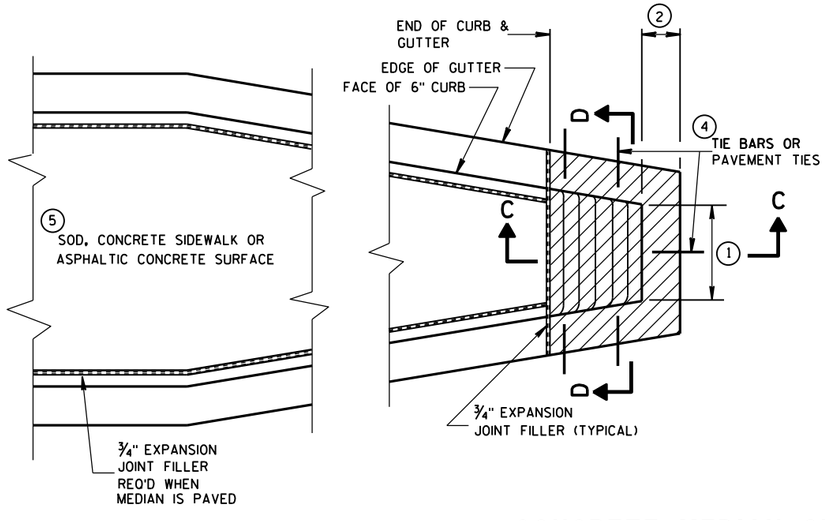
(3) ASPHALTIC CONCRETE PAVEMENT OVER CRUSHED AGGREGATE BASE COURSE.

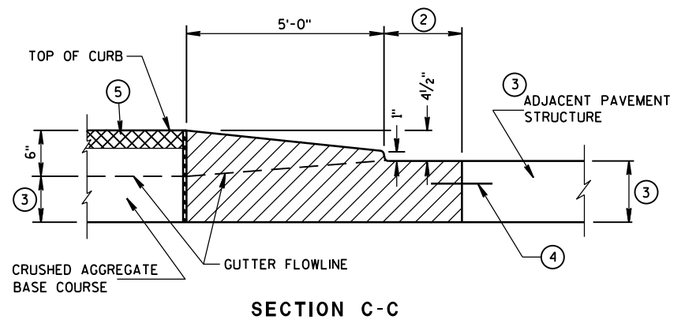
(4) TIE BARS OR PAVEMENT TIES REQUIRED IN NEW CONCRETE PAVEMENT OR CONCRETE BASE COURSE. TIE BARS SHALL BE NO. 4 X 2'-0" SPACED AT 2'-0" C-C.

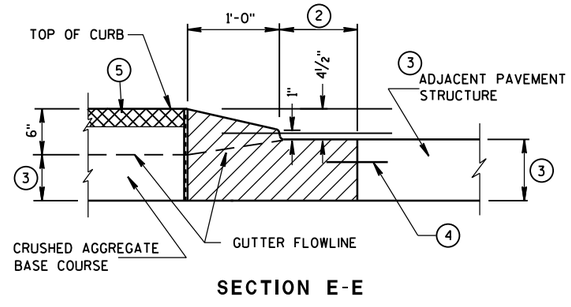
PAVEMENT TIES REQUIRED IN EXISTING CONCRETE BASE COURSE. PAVEMENT TIES SHALL BE NO. 6 X 1'-0" SPACED AT 3'-0" C-C INSTALLED ON A HORIZONTAL SKEW OF 6:1. THE DIRECTION OF SKEW SHALL ALTERNATE AFTER EVERY ONE OR TWO BARS.

(5) SURFACE TYPE AND DETAILS ARE SHOWN ELSEWHERE IN THE PLAN.



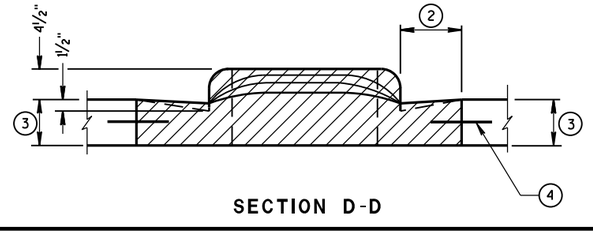






CONCRETE MEDIAN SLOPED NOSE TYPE 2

CONCRETE MEDIAN SLOPED NOSE TYPE 1



CONCRETE MEDIAN NOSE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
6/8/2006
DATE
/S/ Jerry H. Zogg
ROADWAY STANDARDS D ENGINEER
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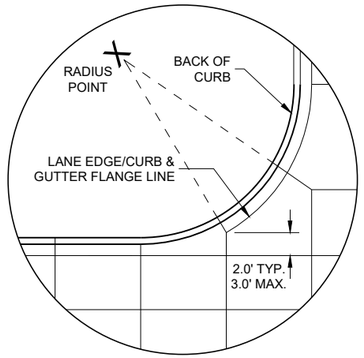
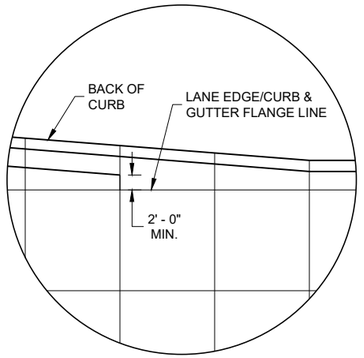
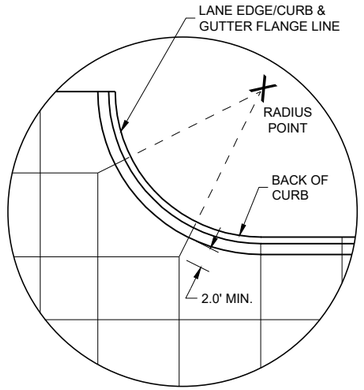


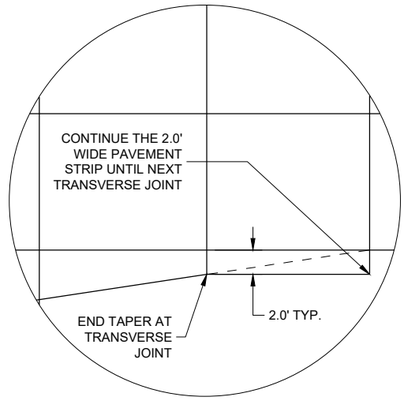
DETAIL "A"



DETAIL "B"



DETAIL "C"



DETAIL "D"

GENERAL NOTES

THE PRIMARY ROADWAY CONTROLS THE TRANSVERSE JOINT PATTERN.

ALIGN NEW JOINTS WITH EXISTING JOINTS OR CRACKS.

CONSTRUCT TRANSVERSE JOINTS PERPENDICULAR TO THE ROADWAY.

ADJUST TRANSVERSE JOINTS TO ALIGN WITH UTILITY FIXTURES (E.G MANHOLES AND INLETS) IN THE PAVEMENT STRUCTURE WHEN POSSIBLE. WATER VALVES DO NOT REQUIRE JOINT ADJUSTMENT.

AVOID SLABS LESS THAN 2 FEET WIDE OR GREATER THAN 15 FEET WIDE.

SEE TABLE FOR TRANSVERSE JOINT SPACING. JOINT SPACING SPECIFIED IS MAXIMUM AND ACTUAL SPACING CAN BE ADJUSTED TO ACCOMMODATE INTERSECTIONS.

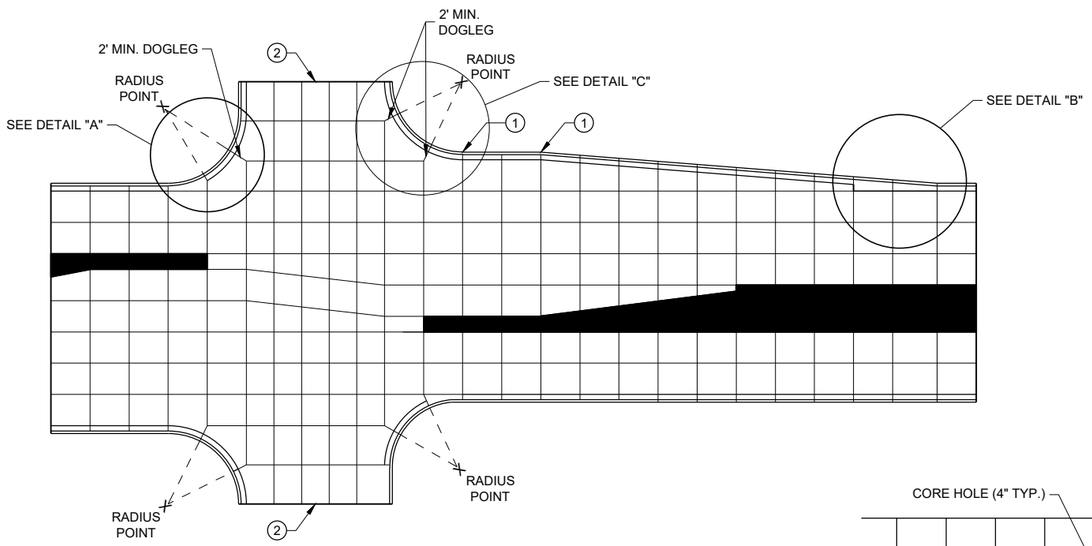
AVOID ANGLES LESS THAN 60° BY DOGLEGGING JOINTS THROUGH CURVE RADIUS POINTS. USE 90° ANGLES WHEN POSSIBLE.

CORRELATE LONGITUDINAL JOINTS WITH LANE LINES WHEN POSSIBLE.

(1) PROVIDE TRANSVERSE JOINTS AT ALL PAVEMENT WIDTH CHANGES.

(2) CONSTRUCT DOWELED EXPANSION JOINT ON THE SIDE ROAD OF AN INTERSECTION IF THE SIDE ROAD IS CONCRETE PAVEMENT AND GREATER THAN 300 FEET IN LENGTH. ALIGN EXPANSION JOINT WITH EDGE OF RADIUS.

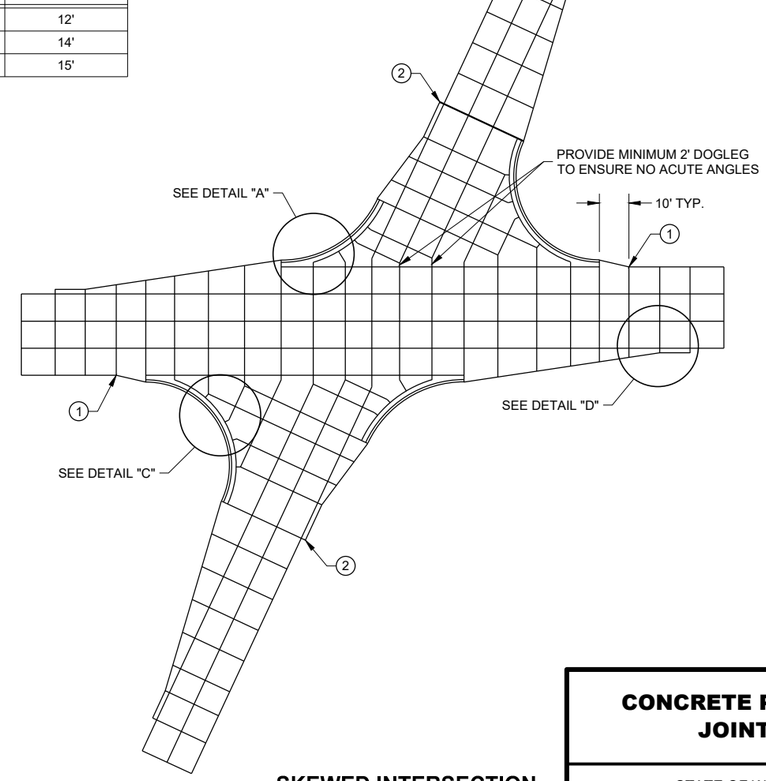
(3) THE ENGINEER MAY APPROVE SLIGHT VARIATIONS FROM THESE JOINTING DETAILS.



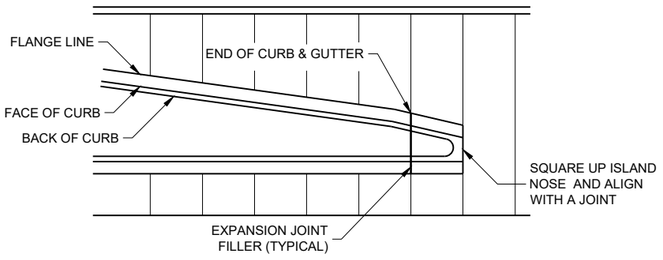
STANDARD INTERSECTION

PAVEMENT DEPTH AND JOINT SPACING TABLE

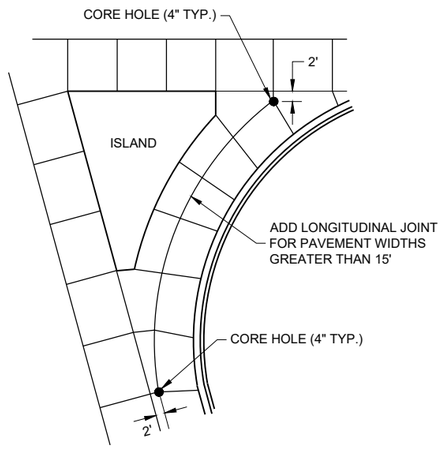
PAVEMENT DEPTH (D)	CONTRACTION JOINT SPACING
6", 6 ½"	12'
7", 7 ½"	14'
8" & ABOVE	15'



SKEWED INTERSECTION



APPROACH TO MEDIAN



LARGE RIGHT TURN

CONCRETE PAVEMENT JOINTING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

SDD 13C18 - 08a

6

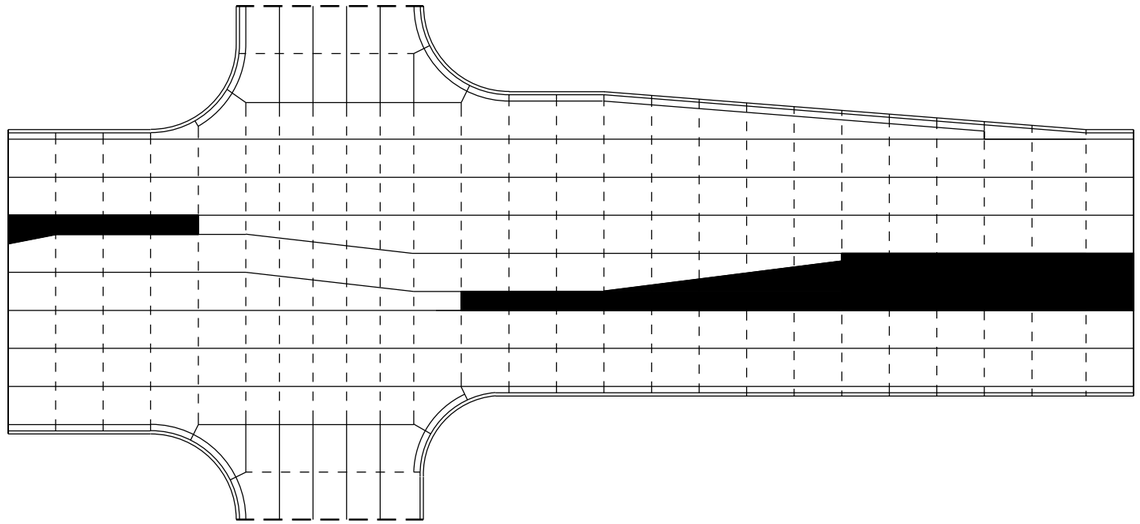
SDD 13C18 - 08a

LEGEND

POTENTIAL DOWELED EXPANSION JOINT

DOWELED JOINT

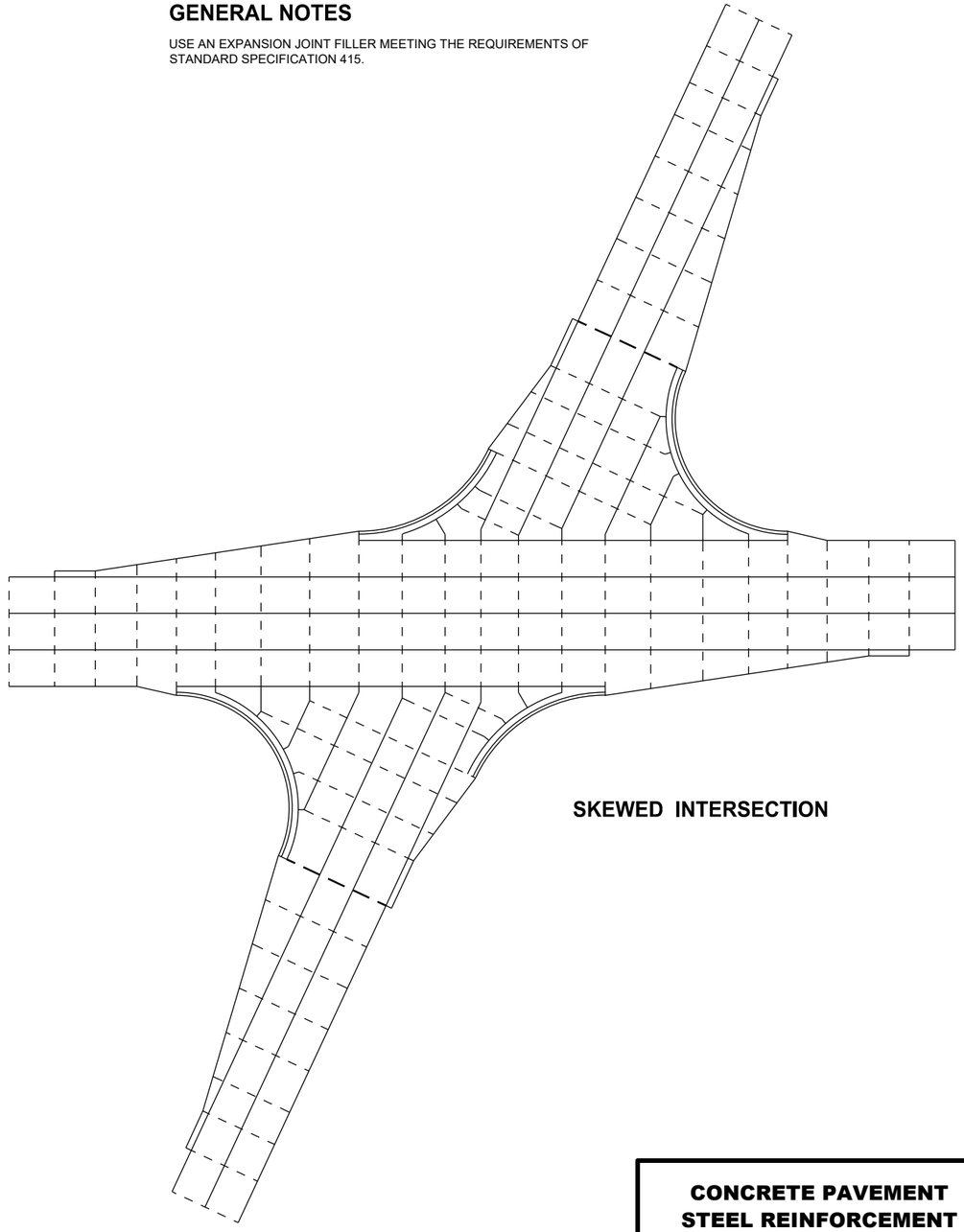
TIED JOINT



STANDARD INTERSECTION

GENERAL NOTES

USE AN EXPANSION JOINT FILLER MEETING THE REQUIREMENTS OF STANDARD SPECIFICATION 415.



SKEWED INTERSECTION

6

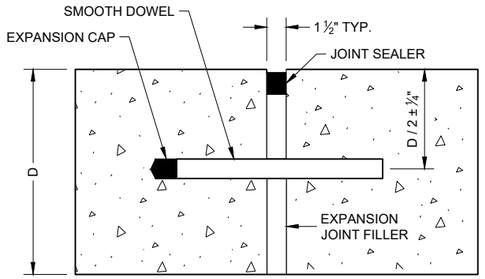
SDD 13C18 - 08b

CONCRETE PAVEMENT STEEL REINFORCEMENT

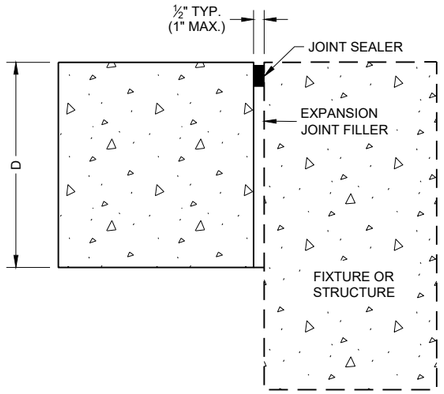
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTA

6

SDD 13C18 - 08b



DOWELED TRANSVERSE (1)



UNTIED - LONGITUDINAL

EXPANSION JOINTS

TIE BAR TABLE

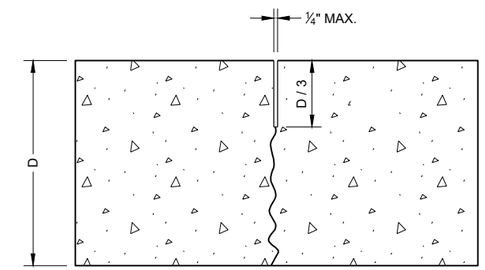
PAVEMENT DEPTH (D)	TIE BAR SIZE	TIE BAR LENGTH (L)	MAX. TIE BAR SPACING
< 10 ½"	NO. 4	30"	36"
≥ 10 ½"	NO. 5	36"	36"
	NO. 4 *	30"	24" **

* SUBSTITUTE BENT BARS AT LONGITUDINAL JOINTS WHEN EQUIPMENT LIMITATIONS DURING CONSTRUCTION WARRANT (e.g. AUXILIARY LANES OR TURN LANES)

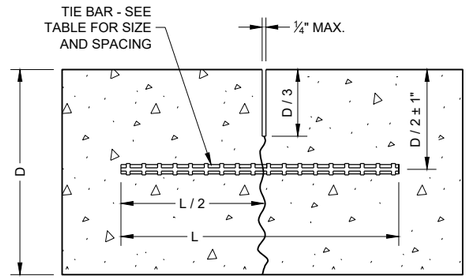
** CONFORM TO 15" MINIMUM SPACING FROM TRANSVERSE JOINTS; SPACING BETWEEN TIE BARS WILL BE 30" AT TRANSVERSE JOINTS.

GENERAL NOTES

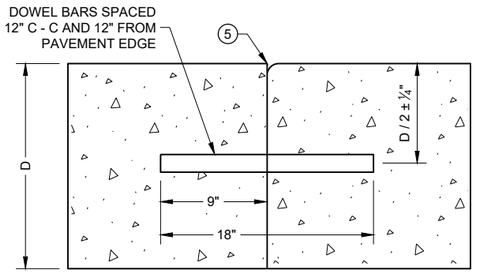
1. USE DOWELED EXPANSION JOINTS ON SIDE ROADS AT INTERSECTIONS (TO ISOLATETHE SIDE ROAD FROM THE THROUGH STREET) IF THE SIDE ROAD IS CONCRETE PAVEMENT AND GREATER THAN 300 FEET IN LENGTH.
2. SPACE CONTRACTION JOINTS IN ACCORDANCE WITH SDD 13C4, 13C11 OR 13C13.
3. LOCATE CONSTRUCTION JOINTS A MINIMUM OF 6 FEET FROM THE NEAREST CONTRACTION JOINT AND ALIGN PARALLEL TO CONTRACTION JOINTS.
4. CONSTRUCTION JOINTS CAN BE FORMED OR SAWED.
5. IF JOINT IS FORMED, PROVIDE A ¼" RADIUS.
6. ANCHOR TIE BARS INTO DRILLED HOLES WITH AN EPOXY.



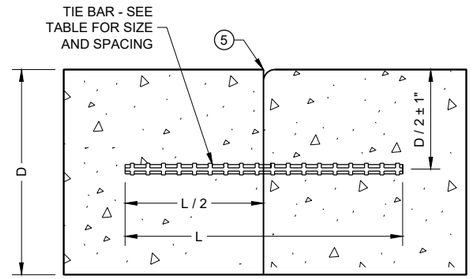
UNDOWELED TRANSVERSE



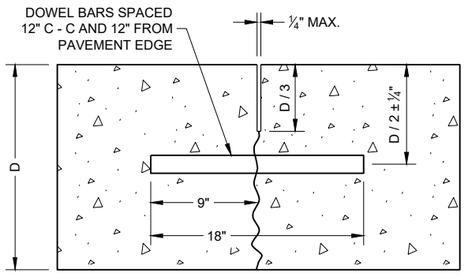
TIED LONGITUDINAL



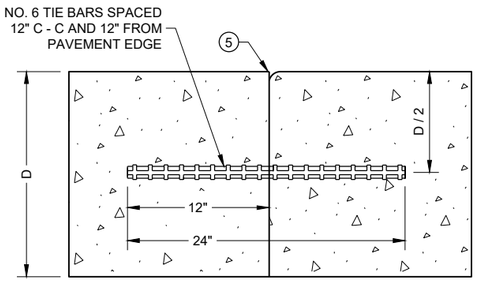
DOWELED TRANSVERSE (3)



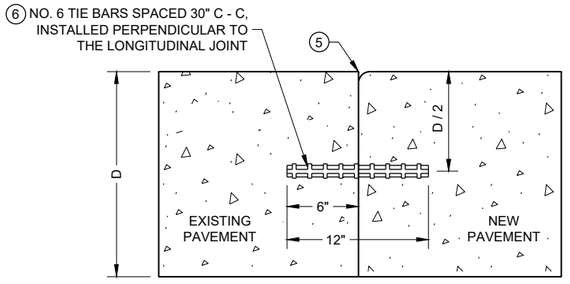
TIED LONGITUDINAL



DOWELED TRANSVERSE



TIED TRANSVERSE (3)
(FOR USE ON NON-DOWELED PAVEMENTS ONLY)

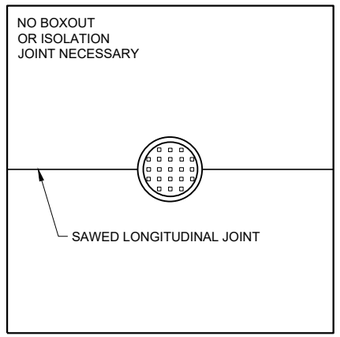


TIED LONGITUDINAL TO EXISTING

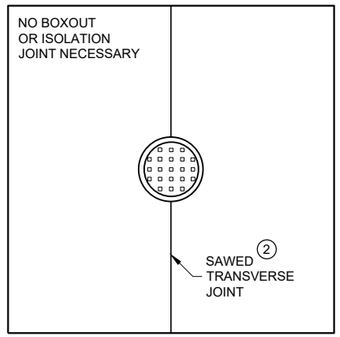
CONTRACTION JOINTS (2)

CONSTRUCTION JOINTS (4)

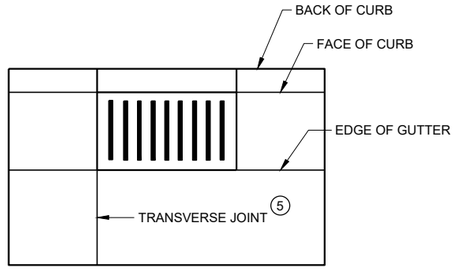
CONCRETE PAVEMENT JOINT TYPES



MANHOLE WITH LONGITUDINAL JOINT



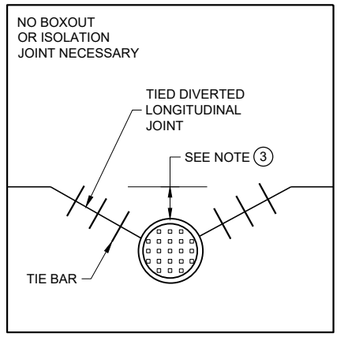
MANHOLE WITH TRANSVERSE JOINT



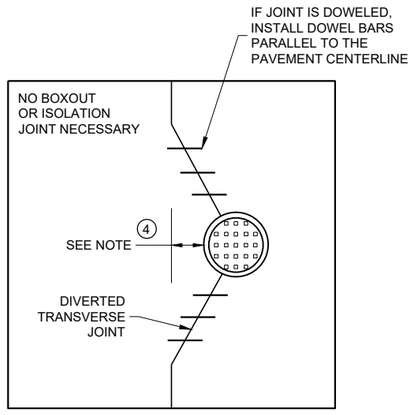
INLET WITH TRANSVERSE JOINT

GENERAL NOTES

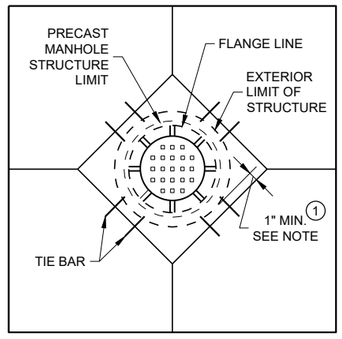
1. USE BOXOUTS WHEN UTILITY STRUCTURE IS IN THE PATH OF CONSTRUCTION JOINTS. PROVIDE A 1 FOOT MINIMUM CLEARANCE BETWEEN THE EXTERIOR LIMIT OF THE STRUCTURE TO THE DIAMOND BOXOUT.
2. ADJUST TRANSVERSE JOINT TO INTERSECT MANHOLE IF POSSIBLE.
3. IF DISTANCE BETWEEN THE LONGITUDINAL JOINT AND THE EDGE OF MANHOLE IS 2 FEET OR LESS, DIVERT THE LONGITUDINAL JOINT AT A 2:1 TAPER RATE TO THE CENTER OF THE MANHOLE. IF THE DISTANCE IS GREATER THAN 2 FEET, DO NOT DIVERT THE JOINT AND SAW AS NORMAL. PLACE REINFORCEMENT REBAR AROUND THE MANHOLE.
4. IF THE DISTANCE FROM THE EDGE OF THE MANHOLE TO THE NEAREST TRANSVERSE JOINT IS LESS 4 FEET OR LESS, REDIRECT JOINT TO INTERSECT THE CENTER OF THE MANHOLE. IF DISTANCE IS GREATER THAN 4 FEET, DO NOT DIVERT THE JOINT AND SAW AS NORMAL. PLACE REINFORCEMENT REBAR AROUND THE MANHOLE.
5. ALIGN TRANSVERSE JOINT WITH ONE EDGE OF INLET WHEN PRACTICAL.



MANHOLE WITH DIVERTED LONGITUDINAL CONTRACTION JOINT



MANHOLE WITH DIVERTED TRANSVERSE CONTRACTION JOINT



DIAGONAL MANHOLE BOXOUT FOR CONSTRUCTION JOINTS

CONCRETE PAVEMENT JOINTING AT UTILITY FIXTURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023
DATE

/S/ Peter Kemp P.E.
PAVEMENT SUPERVISO

FHWA

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SDD13C18 - 08d

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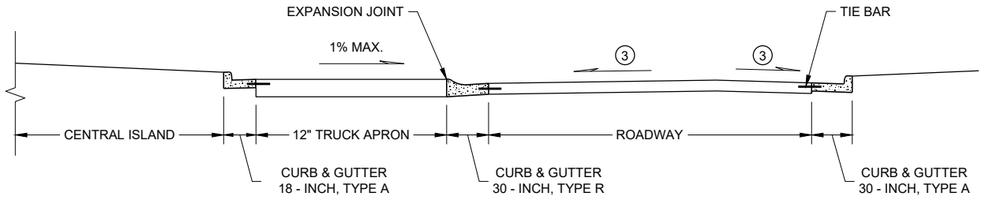
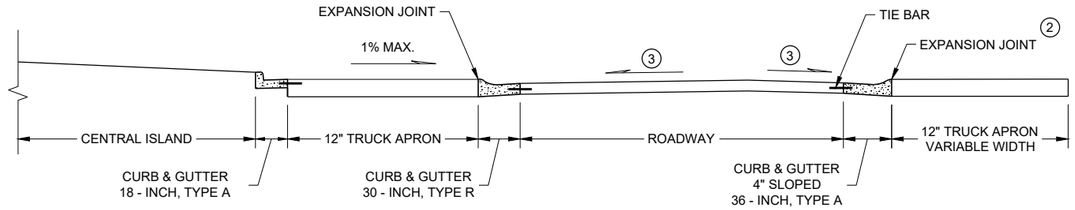
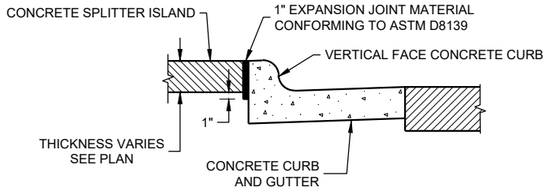
SDD13C18 - 08d



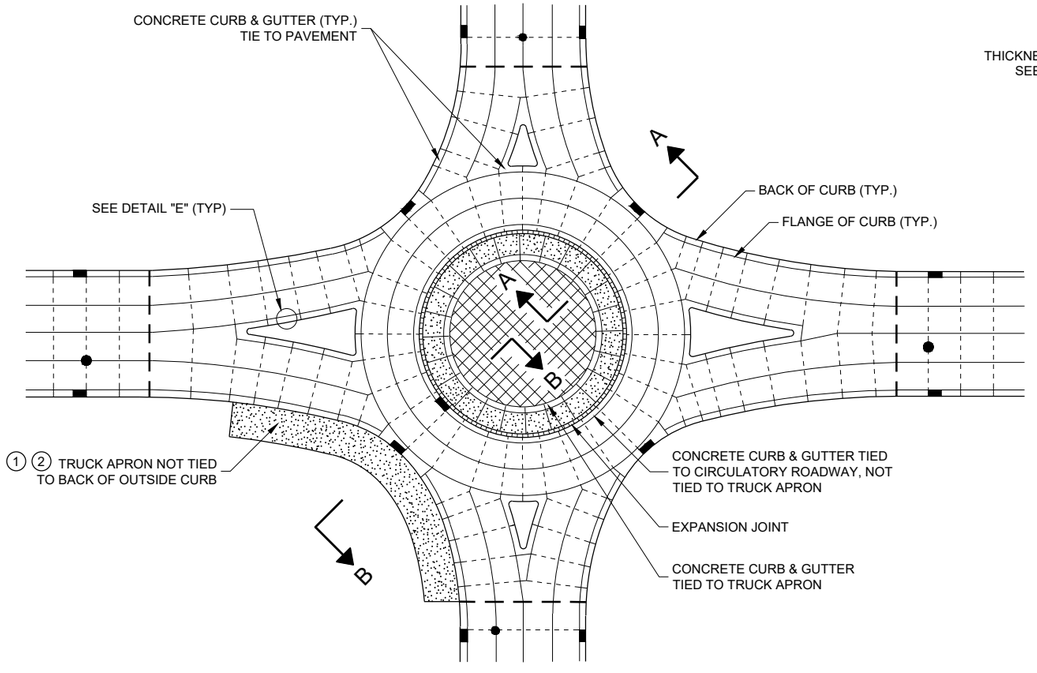
SECTION A - A



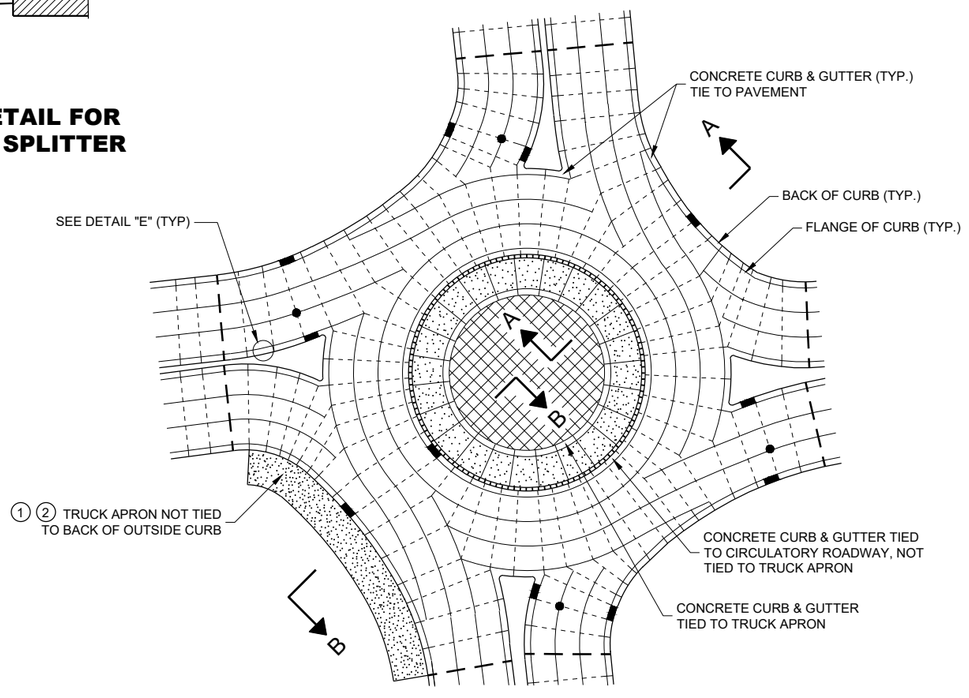
SECTION B - B



DETAIL 'E'
EXPANSION JOINT DETAIL FOR VERTICAL CURB AT A SPLITTER ISLAND



ISOLATED CIRCLE JOINT LAYOUT FOR ROUNDABOUTS



PINWHEEL JOINT LAYOUT FOR ROUNDABOUTS

GENERAL NOTES

MAXIMUM JOINT SPACING IS IN ACCORDANCE WITH THE TABLE SHOWN ON SDD 13C18 - SHEET "a"

USE EXPANSION JOINT FILLER MEETING THE REQUIREMENTS OF STANDARD SPECIFICATION 415.

DO NOT DOWEL OR TIE THE TRUCK APRON TRANSVERSE JOINTS.

(1) DESIGNER DETERMINES SIZE AND LOCATION(S) OF TRUCK APRON TO ACCOMMODATE TRACKING OF OVERSIZE / OVERWEIGHT VEHICLES.

(2) TIE THE OUTSIDE TRUCK APRON TO THE BACK SIDE OF CURB ONLY WHEN ENTIRE TRUCK APRON IS LESS THAN 3 FEET.

(3) CONFORM TO PLAN CONSTRUCTION DETAILS FOR CIRCULATORY ROADWAY CROSS SLOPE.

LEGEND

- DOWELED JOINT
- TIED JOINT
- EXPANSION JOINT
- POTENTIAL DOWELED EXPANSION JOINT
- TRUCK APRON
- CENTRAL ISLAND
- UTILITY STRUCTURES

CONCRETE PAVEMENT JOINTING AND STEEL REINFORCEMENT IN ROUNDABOUTS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023
DATE
/S/ Peter Kemp P.E.
PAVEMENT SUPERVIS

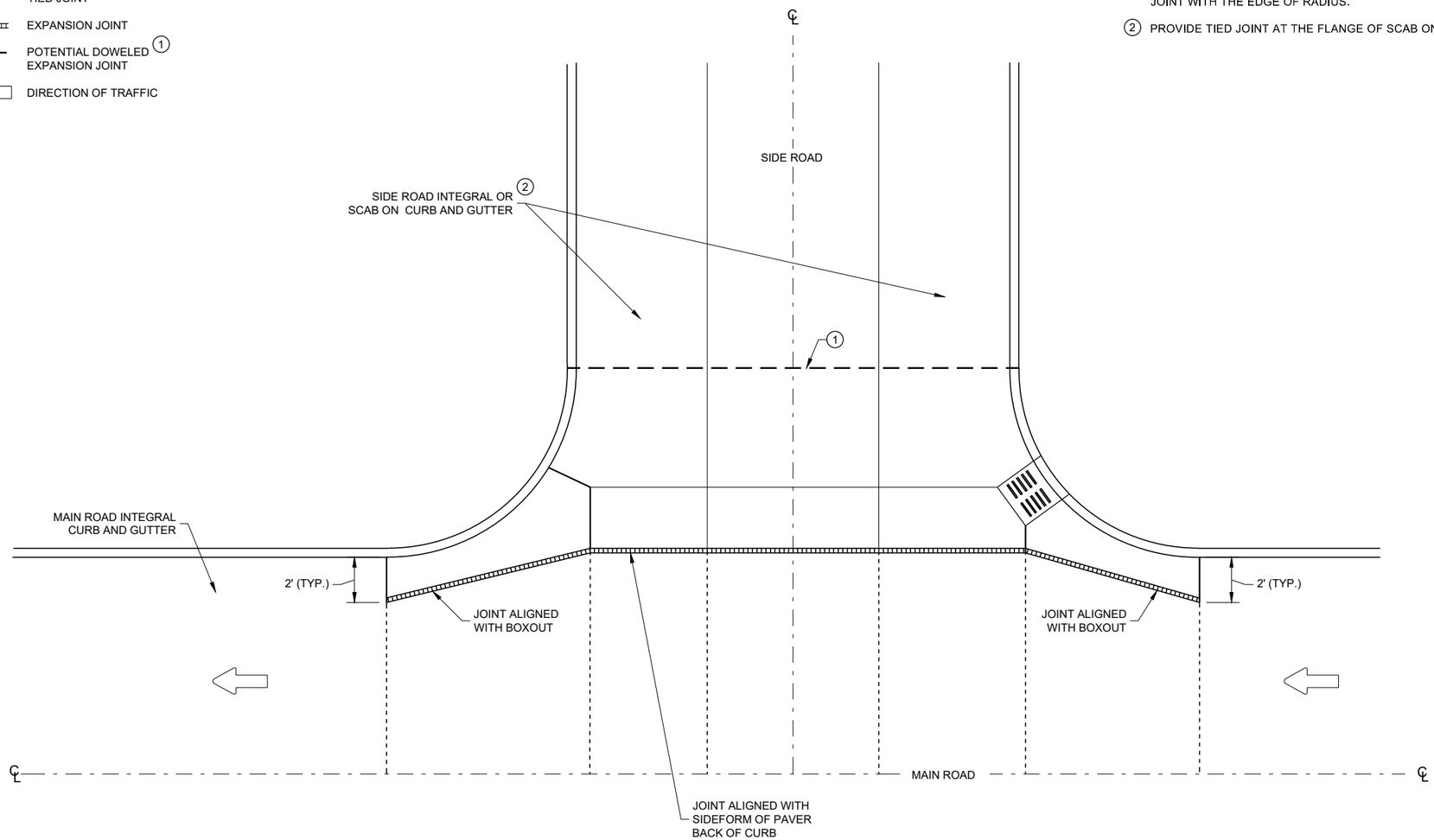
FHWA

6

SDD 13C18 - 08e
SDD 13C18 - 08e

LEGEND

- DOWELED JOINT
- TIED JOINT
- EXPANSION JOINT
- POTENTIAL DOWELED EXPANSION JOINT (1)
- DIRECTION OF TRAFFIC



GENERAL NOTES

(1) CONSTRUCT DOWELED EXPANSION JOINT ON THE SIDE ROAD OF AN INTERSECTION IF THE SIDE ROAD IS CONCRETE PAVEMENT AND GREATER THAN 300 FEET IN LENGTH. ALIGN EXPANSION JOINT WITH THE EDGE OF RADIUS.

(2) PROVIDE TIED JOINT AT THE FLANGE OF SCAB ON CURB IF SCAB ON CURB AND GUTTER IS USE.

INTERSECTION BOXOUT FOR INTEGRAL CURB AND GUTTER

CONCRETE PAVEMENT INTERSECTION BOXOUT FOR INTEGRAL CURB AND GUTTER

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023
DATE

/S/ Peter Kemp P.E.
PAVEMENT SUPERVIS

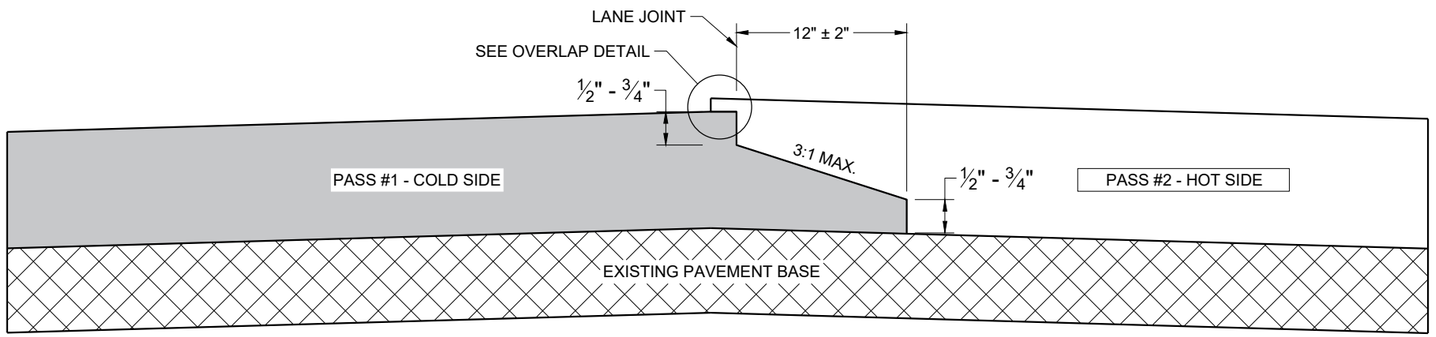
FHWA

6

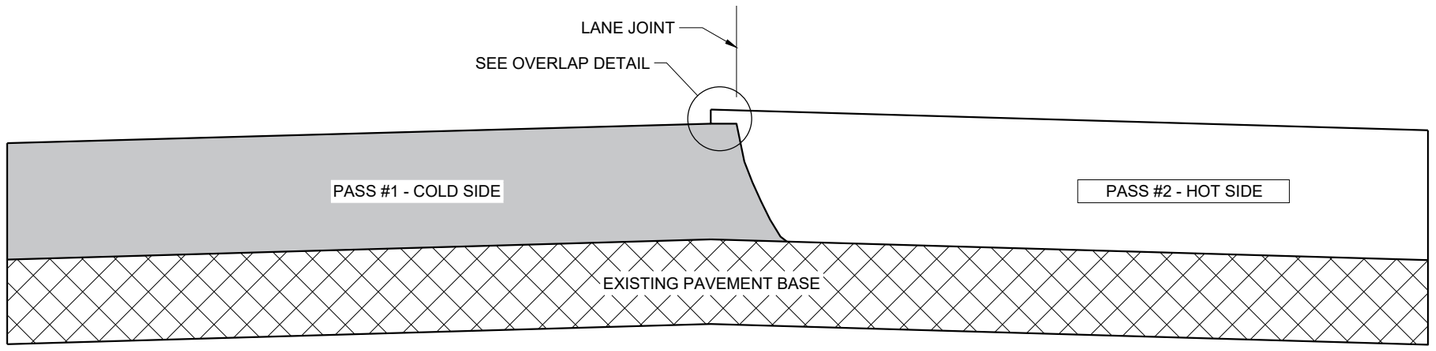
SDD 13C18 - 08f

6

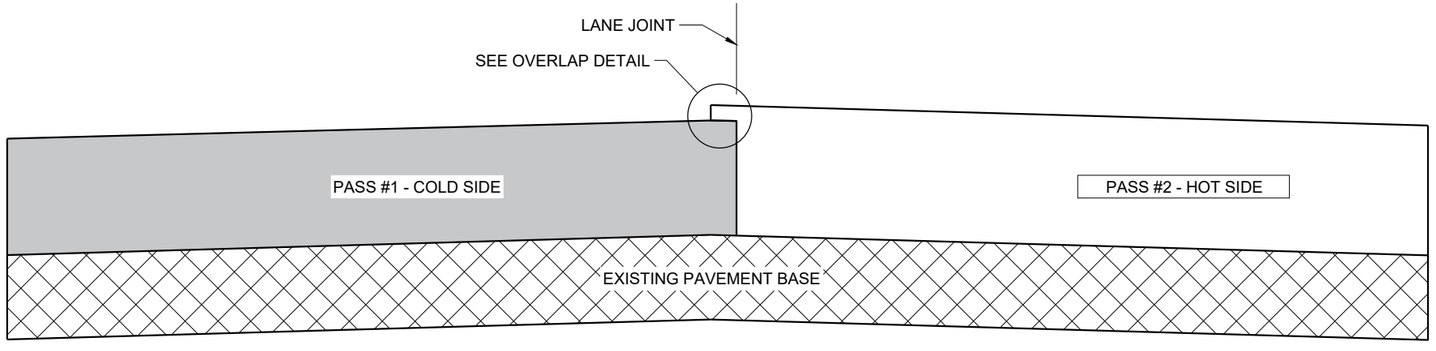
SDD 13C18 - 08f



TYPICAL PAVEMENT CROSS SECTION
NOTCHED WEDGE JOINT



TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT



TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT (MILLED)

GENERAL NOTES

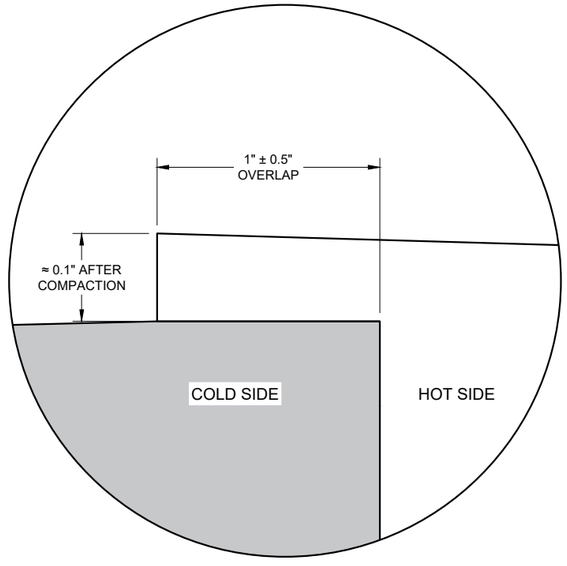
IN ADDITION TO THE DETAILS PROVIDED IN THIS DRAWING, CONFORM TO STANDARD SPECIFICATION 450.3.2.8 FOR WHEN A NOTCHED WEDGE JOINT IS REQUIRED AND FOR GENERAL JOINT CONSTRUCTION REQUIREMENTS.

FOR ALL LONGITUDINAL JOINTS, ENSURE THE PAVER SCREED OVERLAPS THE PREVIOUSLY PLACED PAVEMENT BY 1" ± 0.5" AND THE HOT SIDE OF THE JOINT REMAINS HIGHER THAN THE COLD SIDE BY APPROXIMATELY 0.1" AFTER FINAL COMPACTION. (IT WILL BE FLUSH WHEN PAVING IN ECHELON.)

ONLY REMOVE THE LONGITUDINAL NOTCHED WEDGE JOINT FOR SMA PAVEMENT OR AS DIRECTED BY THE ENGINEER TO ADDRESS SPECIFIC LENGTHS OF JOINT DAMAGED BY TRAFFIC.

WHEN MILLING BACK OR REMOVING ANY LONGITUDINAL JOINT, LIMIT THE MATERIAL REMOVED TO 2" FROM THE TOP NOTCH OR FROM THE VERTICAL JOINT EDGE ON THE COLD SIDE OF THE JOINT.

USE LONGITUDINAL MILLED JOINT AS PLANS SHOW OR THE AS THE ENGINEER DIRECTS.



OVERLAP DETAIL (TYPICAL)

6

6

SDD 13C19 - 03

SDD 13C19 - 03

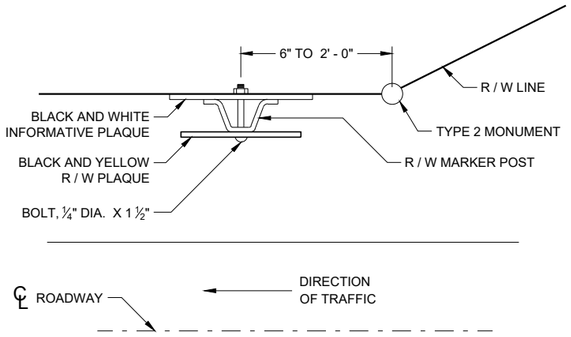
HMA LONGITUDINAL JOINTS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

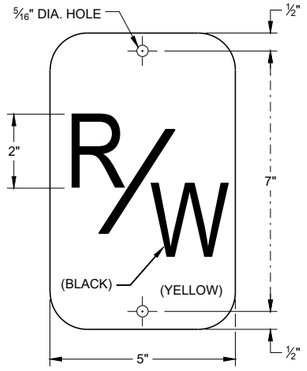
APPROVED
November 2020
DATE

/S/ Steven Hefel
HMA PAVEMENT ENGIN

FHWA

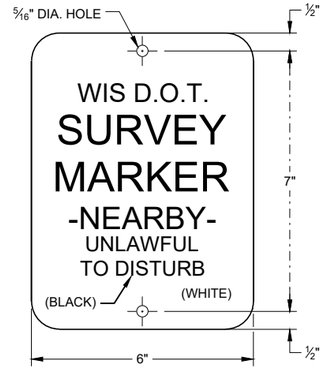


PLAN VIEW
STEEL MARKER POST



R / W PLAQUE

THE RIGHT-OF-WAY PLAQUE AND INFORMATIVE PLAQUE WILL BE FURNISHED BY THE WISCONSIN DEPARTMENT OF TRANSPORTATION.



INFORMATIVE PLAQUE

GENERAL NOTES

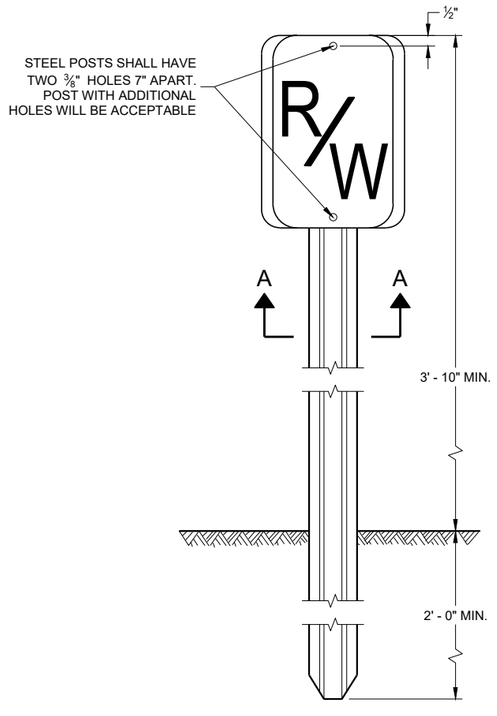
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

A STEEL MARKER POST FOR RIGHT -OF-WAY SHALL BE PLACED IN THE RIGHT-OF-WAY WITH THE BACK OF THE POST ON THE LONGER RIGHT-OF-WAY TANGENT, 6 INCHES TO 24 INCHES FROM EACH TYPE 2 MONUMENT TO SERVE AS A GUARD POST, AND AT OTHER LOCATIONS AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.

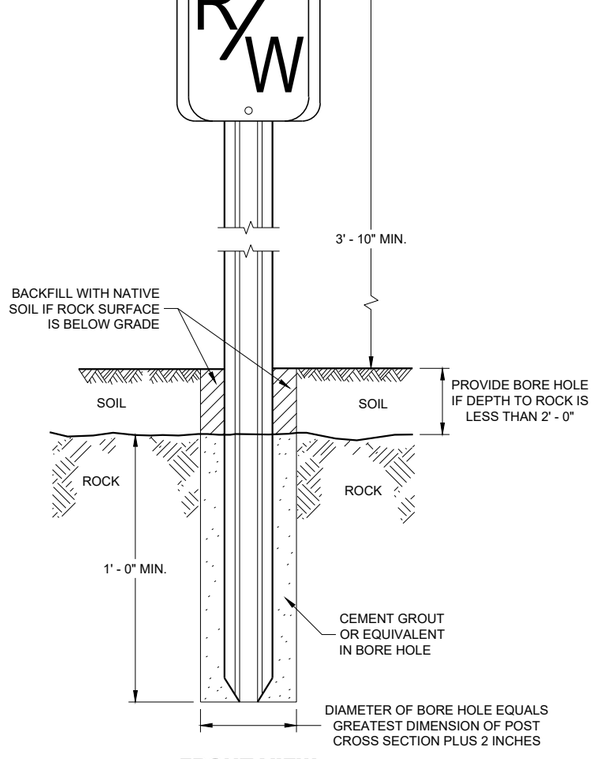
THE "R/W" PLAQUE SHALL FACE THE ROADWAY AND THE INFROMATIVE PLAQUE SHALL FACE AWAY FROM THE ROADWAY. "R/W" AND INFORMATIVE PLAQUES WILL BE FURNISHED BY THE WISCONSIN DEPARTMENT OF TRANSPORTATION.

STEEL MARKER POSTS SHALL MEET THE MINIMUM MATERIAL REQUIREMENTS FOR STEEL DELINEATOR POSTS; EXCEPT POSTS PAINTED WITH FEDERAL YELLOW ENAMEL NEED NOT BE ZINC COATED.

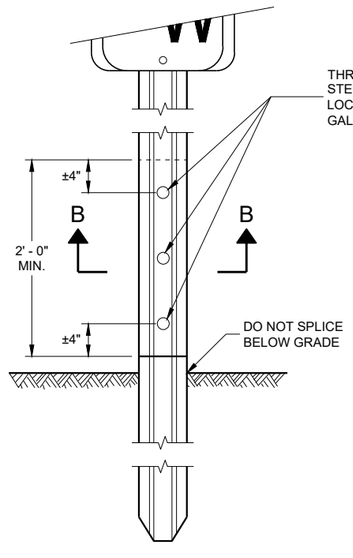
(1) IN AREAS OF SOLID ROCK, DRILL A BORE HOLE 2" GREATER THAN THE WIDEST DIMENSION OF THE POST CROSS SECTION INTO THE ROCK A MINIMUM DEPTH OF 12 INCHES. CUT OR SPLICE THE POST SO THAT A MINIMUM LENGTH OF 3' - 10" PROTRUDES ABOVE THE GROUND. BLOW OUT THE BORE HOLE IN THE ROCK USING COMPRESSED AIR. FILL THE BORE HOLE WITH CEMENT GROUT OR EQUIVALENT, DEPENDING ON THE STABILITY OF THE ROCK.



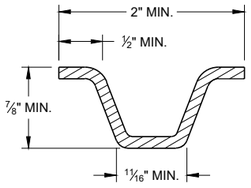
FRONT VIEW
STEEL MARKER POST



FRONT VIEW
ROCK INSTALLATION (1)

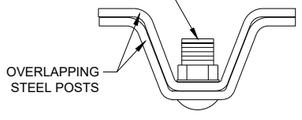


FRONT VIEW
SPLICE DETAIL



MIN. WEIGHT 1.12 LB./FT.

SECTION A - A



SECTION B - B

MARKER POST FOR RIGHT - OF - WAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
2/18/2016
DATE
/S/ Ray Kumapayi
CHIEF SURVEYING AND MA
ENGINEER

FHWA

6

SDD 15A01 - 13a

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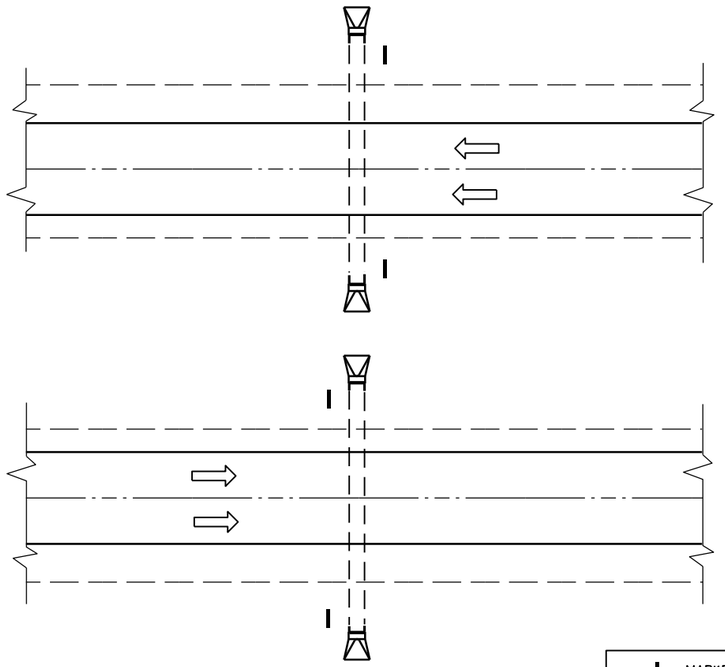
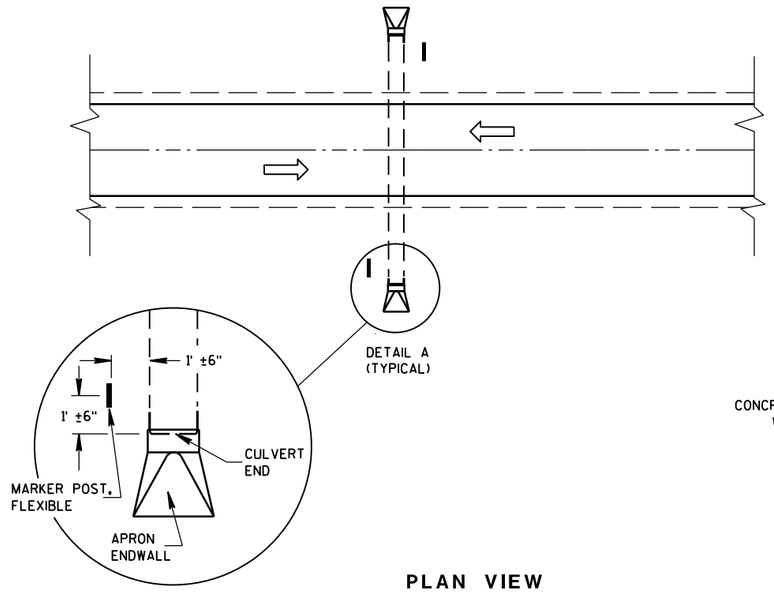
SDD15A01 - 13a



PLAN VIEW

DIVIDED HIGHWAY



DETAIL A
(TYPICAL)

PLAN VIEW

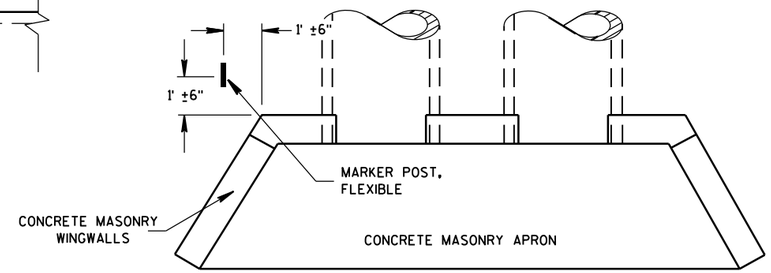
UNDIVIDED HIGHWAY

MARKER POST, FLEXIBLE

DIRECTION OF TRAFFIC FLOW

GENERAL NOTES

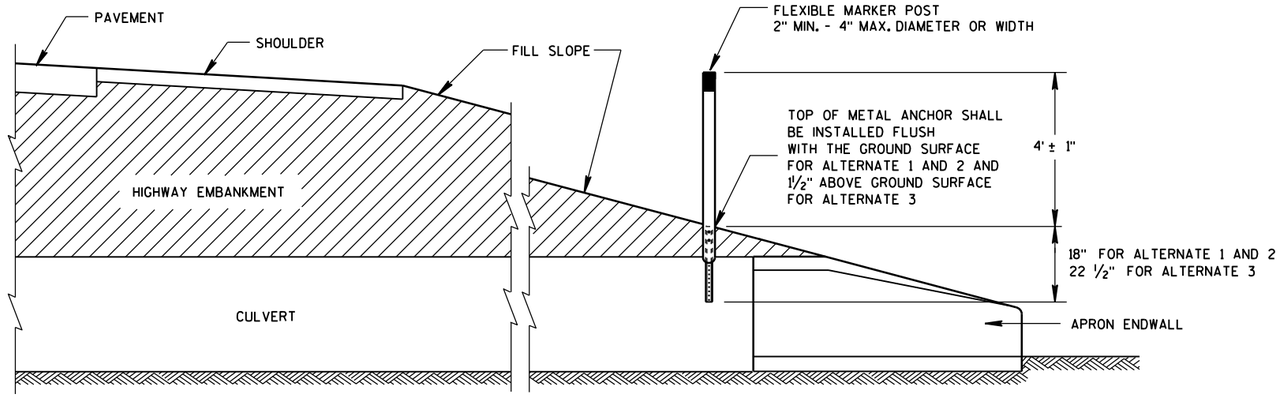
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.



PLAN VIEW

CONCRETE MASONRY ENDWALLS FOR CULVERT PIPE AND PIPE ARCH

FLEXIBLE MARKER POST LOCATION



CROSS SECTION

FLEXIBLE MARKER POST

FLEXIBLE MARKER POST
FOR CULVERT END

STATE OF WISCONSIN
DEPARTMENT OF TRANSPOI

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S.D.D. 15 A 3-2a

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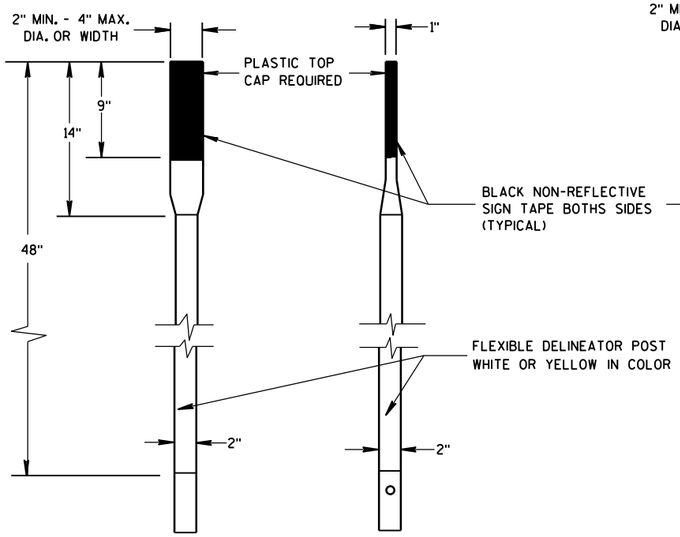
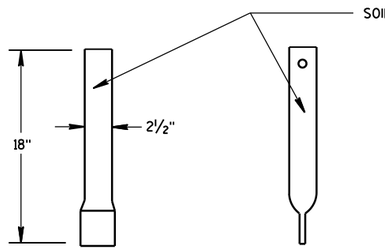
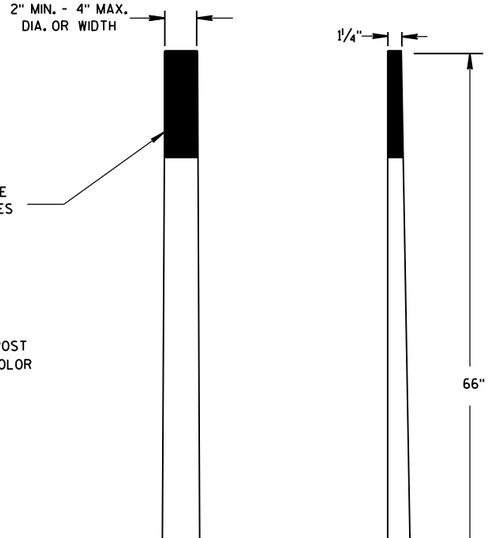
S.D.D. 15 A 3-2a





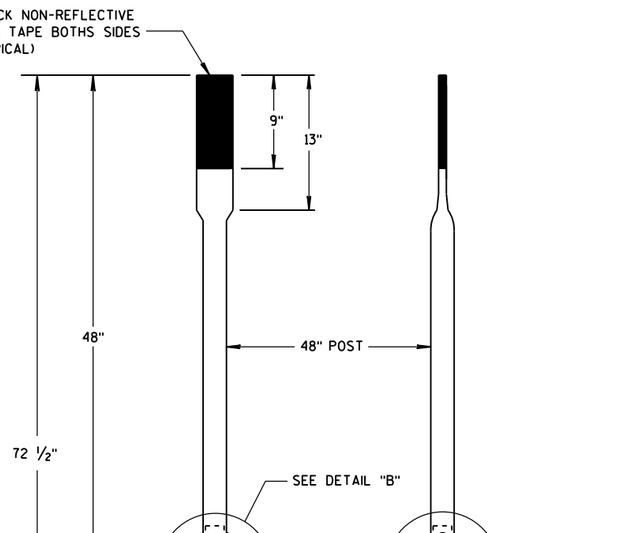
FRONT VIEW SIDE VIEW

ALTERNATE 1



FRONT VIEW SIDE VIEW

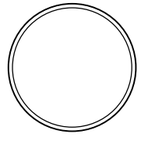
ALTERNATE 2



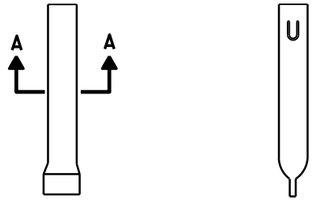
FRONT VIEW SIDE VIEW

ALTERNATE 3

FLEXIBLE MARKER POSTS

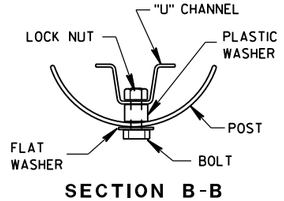


SECTION A-A

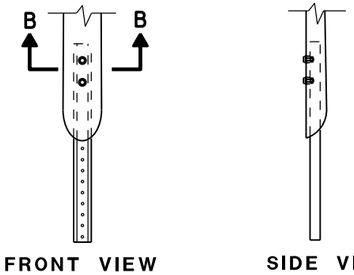


FRONT VIEW SIDE VIEW

ALTERNATE 1

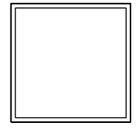


SECTION B-B

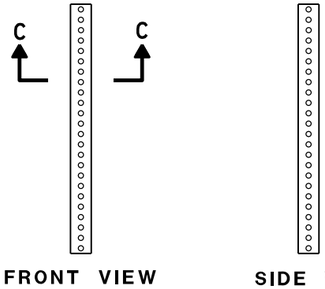


FRONT VIEW SIDE VIEW

ALTERNATE 2



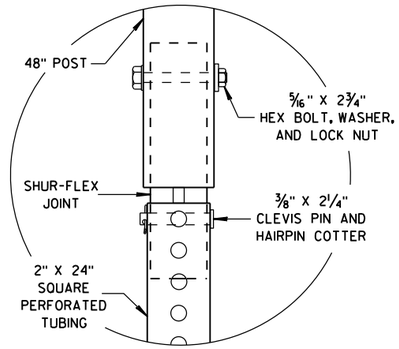
SECTION C-C



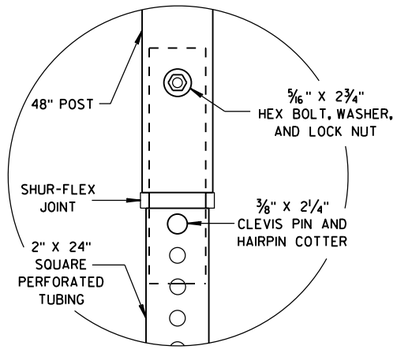
FRONT VIEW SIDE VIEW

ALTERNATE 3

FLEXIBLE MARKER POST ANCHORS



DETAIL B



DETAIL C

FLEXIBLE MARKER POST
FOR CULVERT END

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
10/1/2012
DATE
/S/ Travis Feltes
STATE TRAFFIC ENGINEER
FHWA

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S.D.D. 15 A 3-2b

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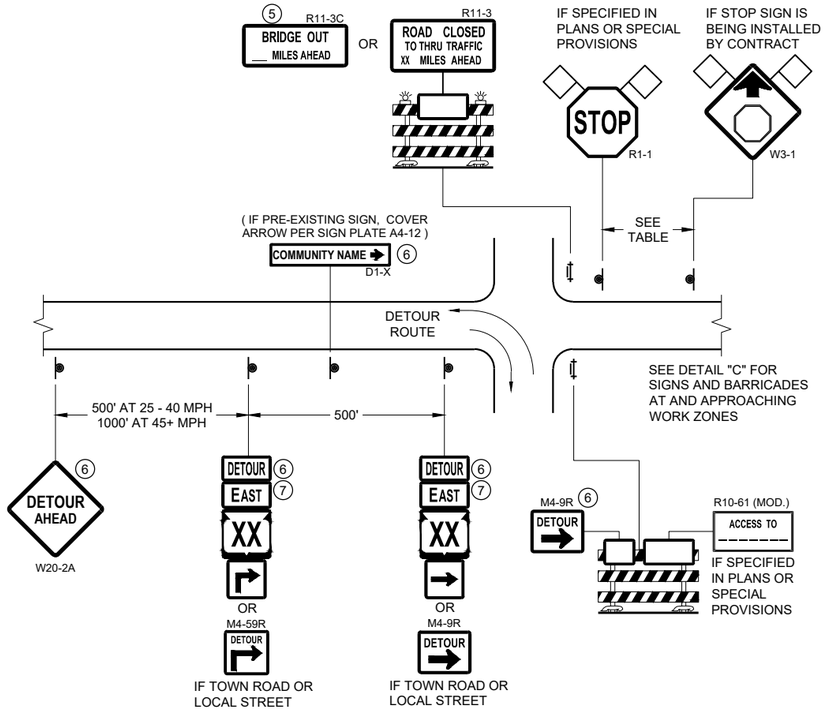
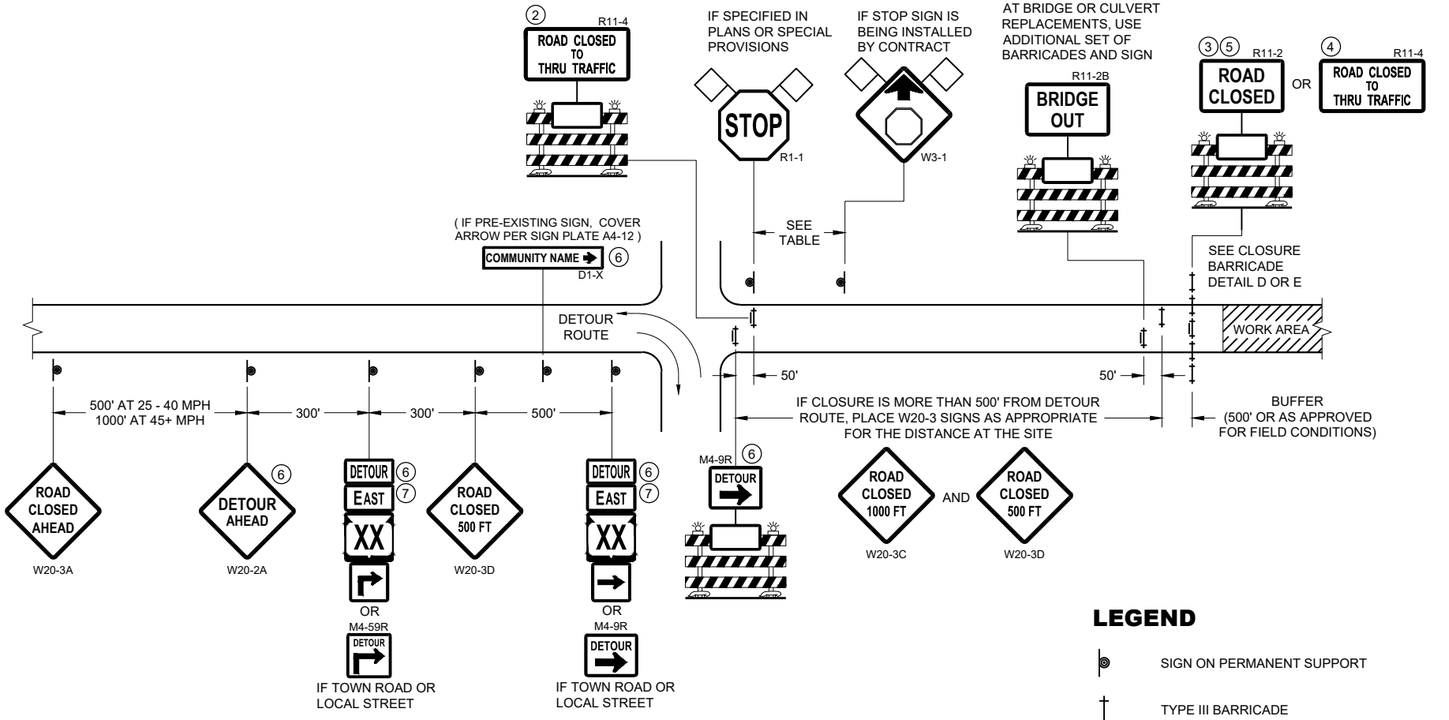
S.D.D. 15 A 3-2b



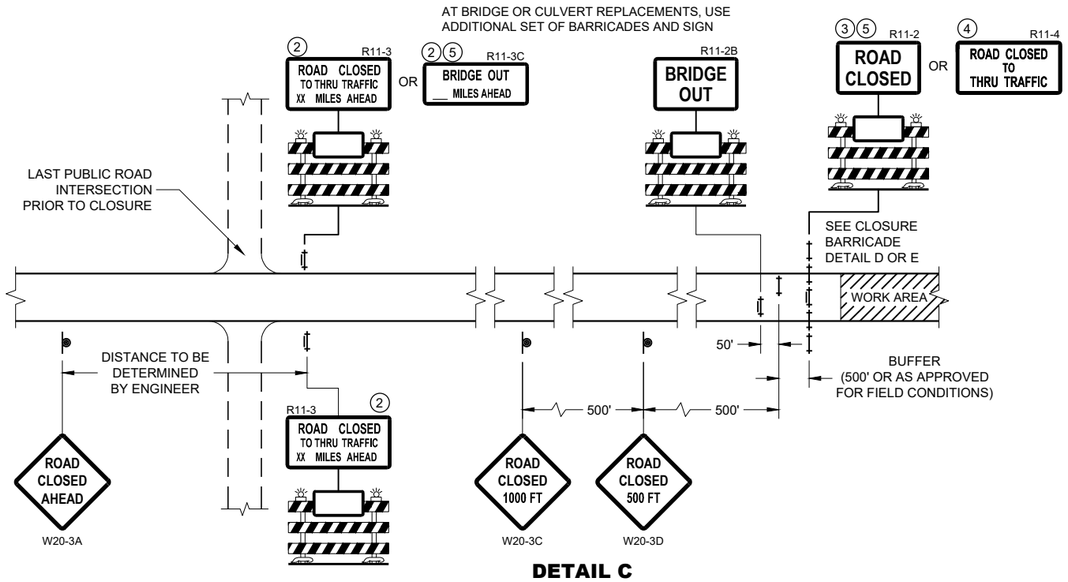
DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR

WORK ZONE GREATER THAN OR EQUAL TO ½ MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)



DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR

WORK ZONE LESS THAN ½ MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)



DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

SEE SDD 15C2-SHEET "b"
FOR GENERAL NOTES
AND FOOTNOTES ① THROUGH ⑦

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
- FLAGS, 16" X 16" MIN. (ORANGE)
- DETOUR — M4 - 8
- EAST — M3 - X
- XX — M1 - 4 OR XX — M1 - 6 OR COUNTY X — M1 - 5A
- M05 - 1 OR M06 - 1

BARRICADES AND SIGNS FOR MAINLINE CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023
DATE
/S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

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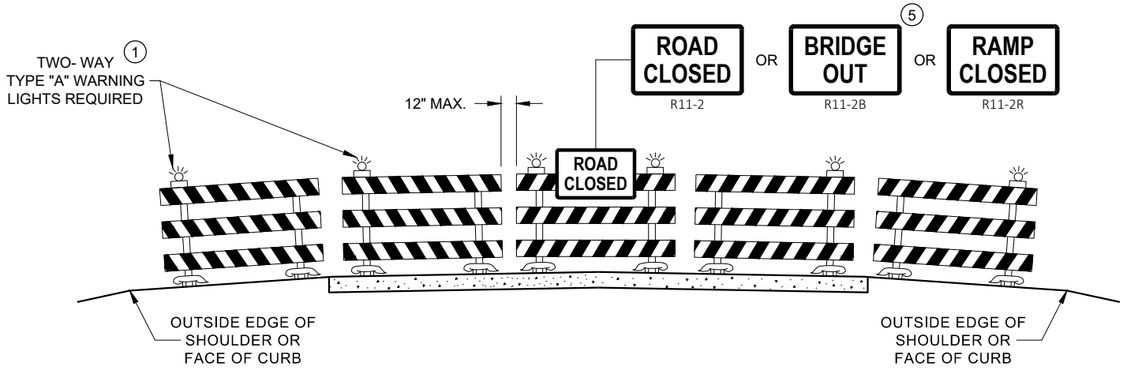
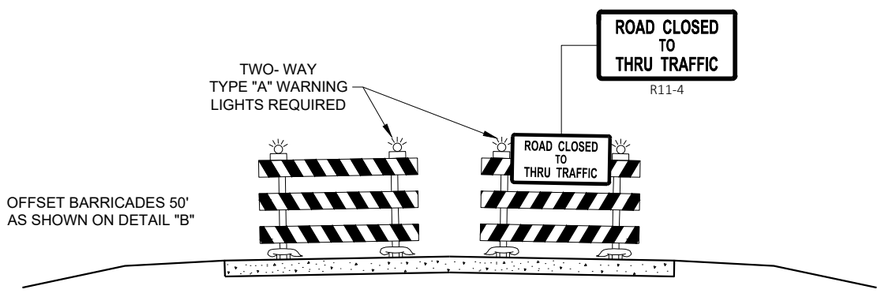
SDD 15C02 - 09a

SDD 15C02 - 09a



DETAIL D
ROAD CLOSURE BARRICADE DETAIL
APPROACH VIEW



DETAIL E
LANE CLOSURE BARRICADE DETAIL
APPROACH VIEW

SEE SDD 15C2 - SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW - INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11 - 2, R11 - 3, M4 - 9, R11 - 4, AND R10 - 61 SIGNS PLACED ON THE BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE RAIL OR BOTTOM RAILS.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
- R11 - 2 SHALL BE 48" X 30"
- R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60 " X 30"
- M4 - 9 SHALL BE 30" X 24"
- M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1 - 1 SHALL BE 36" X 36"

(1) TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT SPACING.

(2) THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.

(3) FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".

(4) FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".

(5) FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.

(6) INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.

(7) "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

BARRICADES AND SIGNS FOR VARIOUS CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023
DATE
/S/ Andrew Heidtke
WORK ZONE ENGINEE

FHWA

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SDD 15C02 - 09b

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SDD 15C02 - 09b

THIS DRAWING PROVIDES GENERAL GUIDANCE ON TYPICAL DETOUR SIGN LAYOUT AND SPACING. SEE PROJECT DETOUR SIGNING SHEETS FOR SPECIFIC DETAILS FOR EACH PROJECT.

LEGEND

- SIGN ON PERMANENT SUPPORT
- WORK AREA
- DETOUR / EAST — M4 - 8, M3 - X
- XX — M1 - 4; OR XX — M1 - 6; OR COUNTY X — M1 - 5A
- M05 - 1; OR M06 - 1; OR M06 - 1

GENERAL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. MODIFY EXISTING SIGNS WHERE POSSIBLE.

THE SPACING BETWEEN TRAFFIC CONTROL AND DETOUR SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

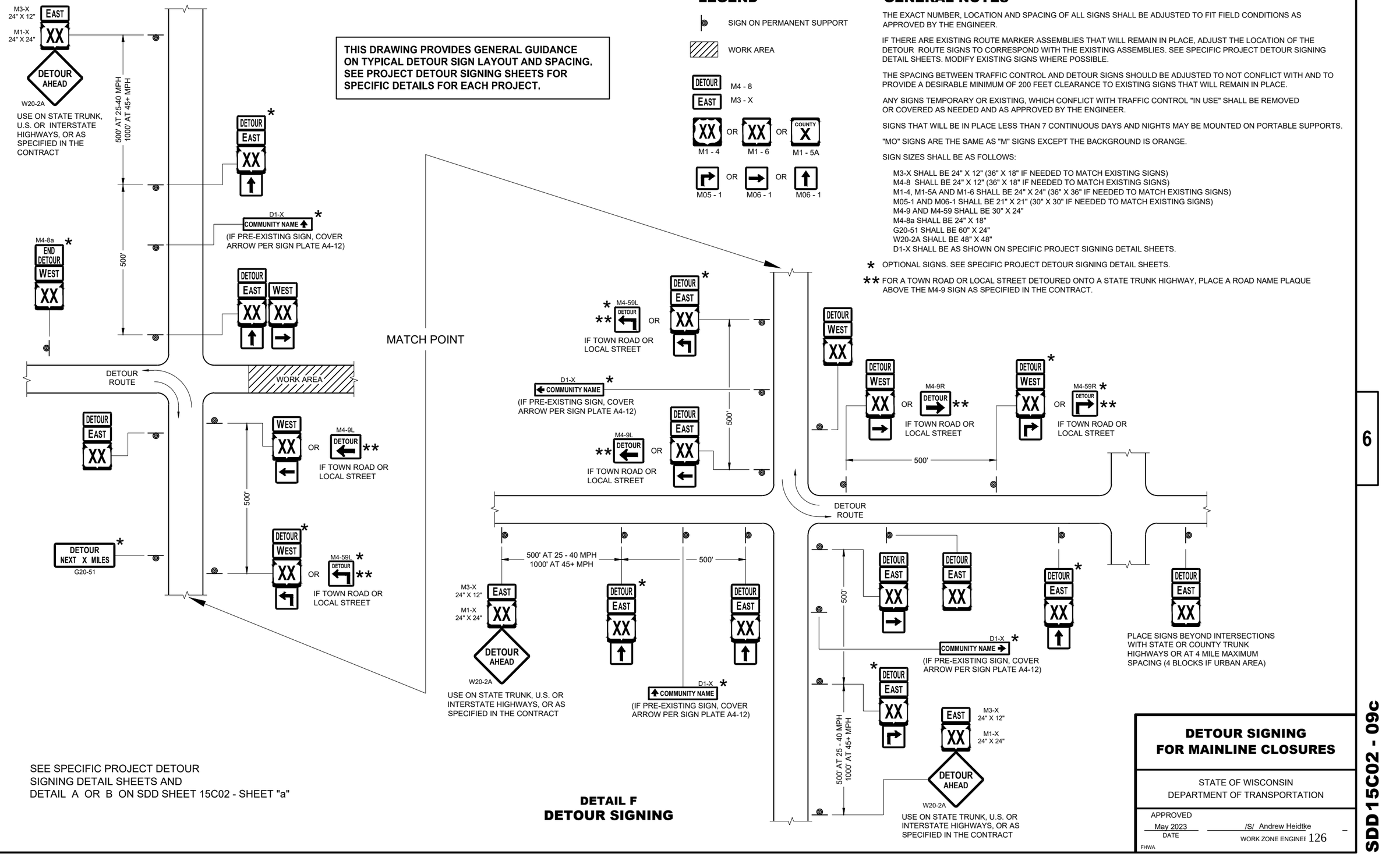
"MO" SIGNS ARE THE SAME AS "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGN SIZES SHALL BE AS FOLLOWS:

- M3-X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4-8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1-4, M1-5A AND M1-6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- M05-1 AND M06-1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- M4-9 AND M4-59 SHALL BE 30" X 24"
- M4-8a SHALL BE 24" X 18"
- G20-51 SHALL BE 60" X 24"
- W20-2A SHALL BE 48" X 48"
- D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.

* OPTIONAL SIGNS. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS.

** FOR A TOWN ROAD OR LOCAL STREET DETOURED ONTO A STATE TRUNK HIGHWAY, PLACE A ROAD NAME PLAQUE ABOVE THE M4-9 SIGN AS SPECIFIED IN THE CONTRACT.



SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS AND DETAIL A OR B ON SDD SHEET 15C02 - SHEET "a"

DETAIL F
DETOUR SIGNING

DETOUR SIGNING FOR MAINLINE CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

6

SDD 15C02 - 09c

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

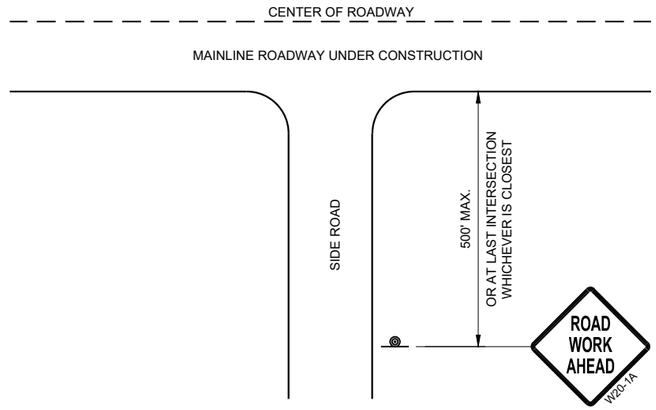
ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

* OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS

** PLACE AN ADDITIONAL W20-1A "ROAD WORK AHEAD" SIGN IF WORK AREA WITHIN THE PROJECT IS SEPARATED BY MORE THAN 2 MILES FROM PREVIOUS WORK AREA.



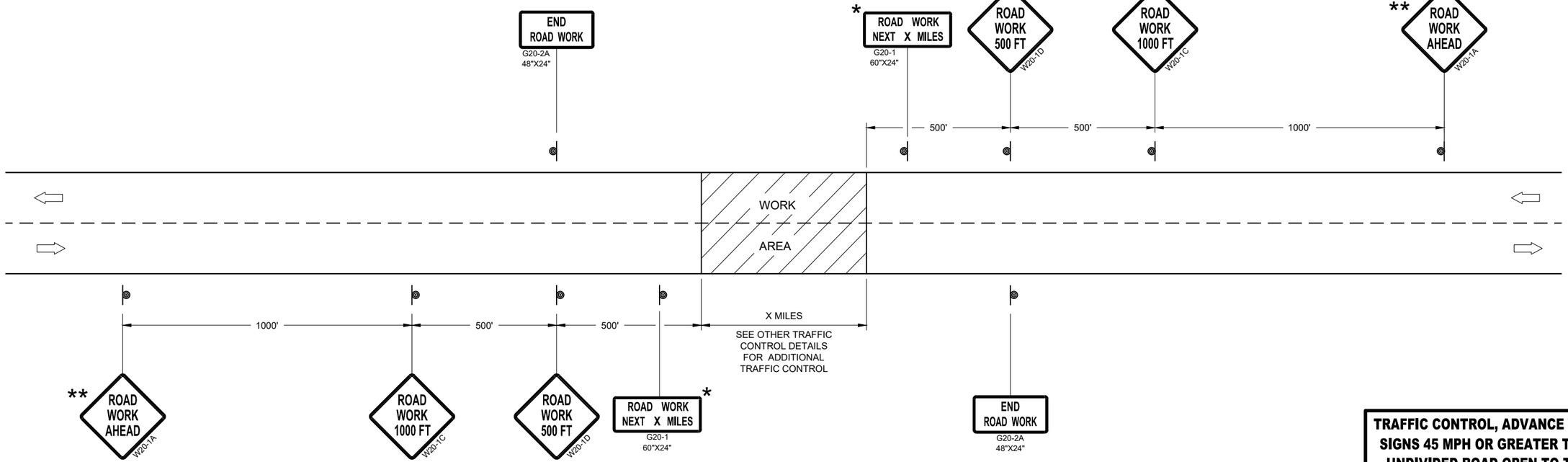
TYPICAL SIDE ROAD APPROACH WARNING SIGN DETAIL

LEGEND

SIGN ON PERMANENT SUPPORT

DIRECTION OF TRAFFIC

WORK AREA



TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45MPH OR GREATER

TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 MPH OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
July 2018
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEE

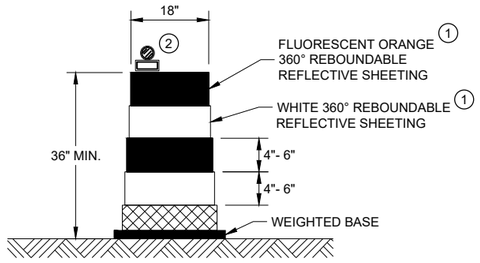
FHWA

6

SDD 15C04 - 05

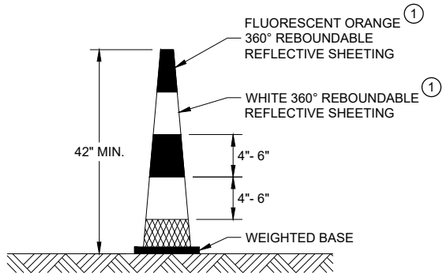
GENERAL NOTES

1. REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
2. LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.



DRUM

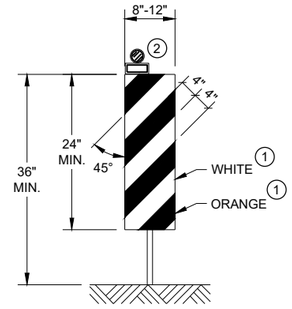
BALLAST WIDTHS
RANGE FROM 24"-36"



42" CONE

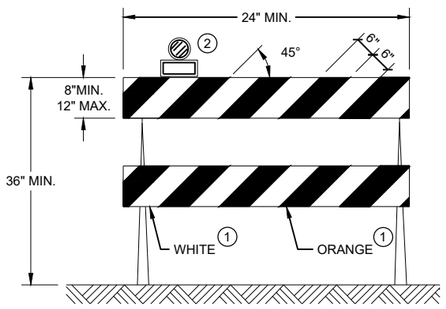
DO NOT USE IN TAPERS
½ SPACING OF DRUMS

BALLAST WIDTHS
RANGE FROM 14"-20"



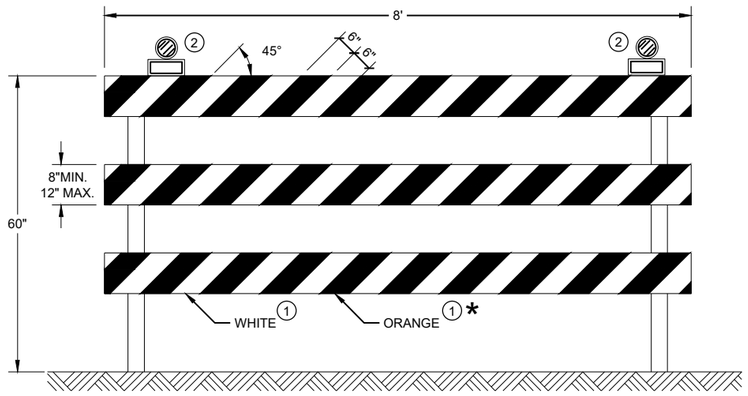
VERTICAL PANEL

THE STRIPES SHALL SLOPE DOWNWARD TO
THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE II BARRICADE

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES
MAY BE USED. ALL STRIPES SHALL SLOPE DOWNWARD
TO THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE III BARRICADE

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP
TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.

* IF USED FOR A PERMANENT APPLICATION USE RED SHEETING.

CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2022
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEE

FHWA

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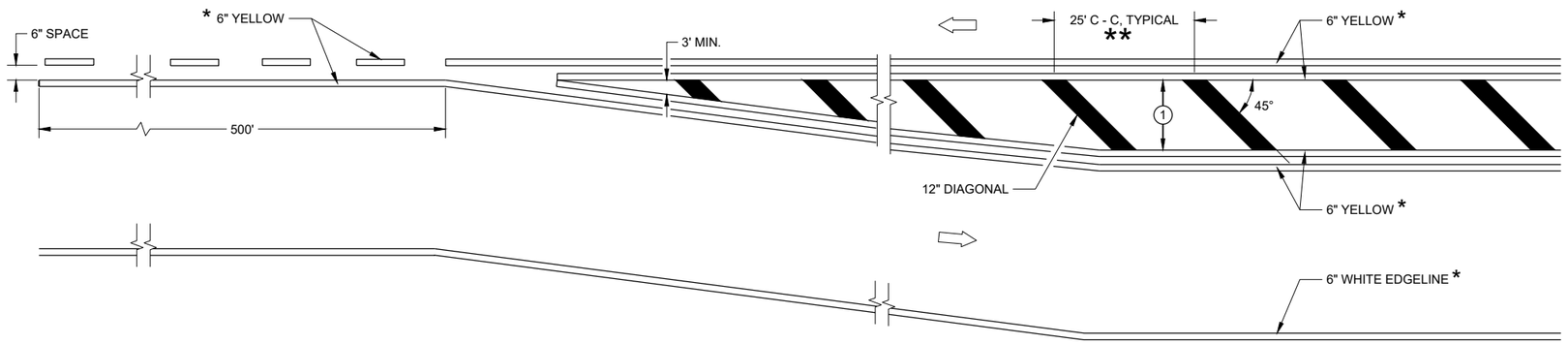
SDD 15C11 - 10b

6

SDD 15C11 - 10b



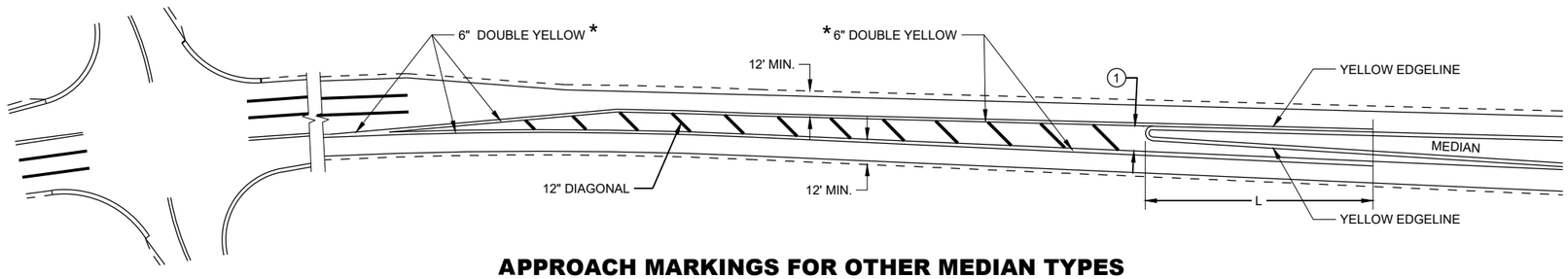
MEDIAN ISLAND DETAIL

GENERAL NOTES

1. DIAGONALS ARE OPTIONAL WHEN PAINTED ISLAND IS LESS THAN 6 FEET AT THE WIDEST POINT. OMIT DIAGONALS IF WIDTH IS LESS THAN 4 FEET.

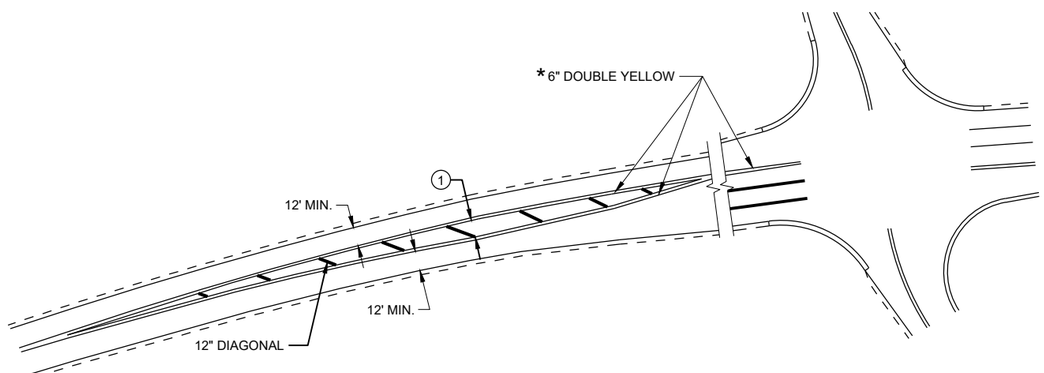
⇨ DIRECTION OF TRAVEL

* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES



APPROACH MARKINGS FOR OTHER MEDIAN TYPES

SPEED LIMIT	L
<35 MPH	5'
35> MPH	50'



NON-APPROACH MARKINGS

MEDIAN ISLAND PAVEMENT MARKINGS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

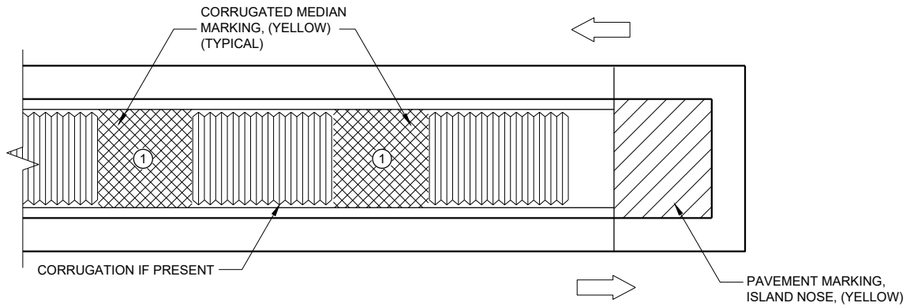
APPROVED
May 2023 — DATE
/S/ Jeannie Silver — STATE SIGNING AND MAF ENGINEER
FHWA

6

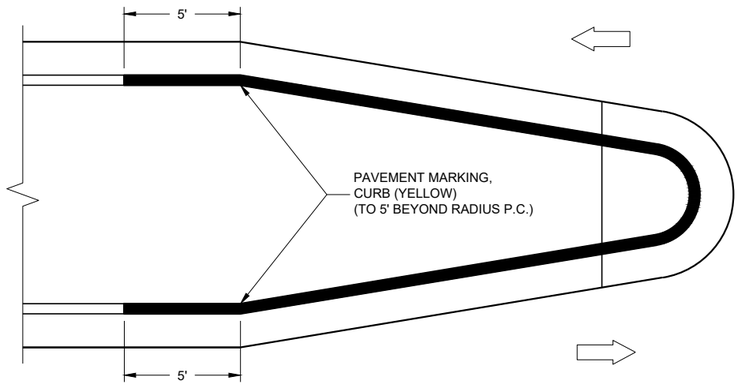
SDD 15C18-08a

6

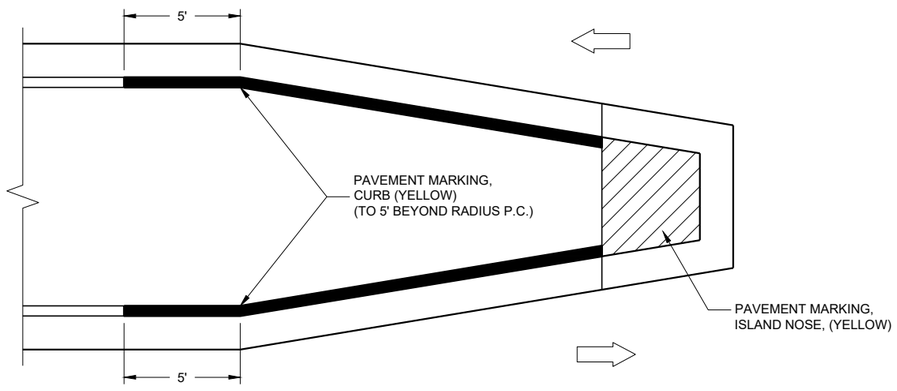
SDD 15C18-08a



MEDIAN ISLAND WITH SQUARE BLUNT NOSE



MEDIAN ISLAND WITH ROUND BLUNT NOSE



MEDIAN ISLAND WITH SLOPED NOSE

TYPICAL PLACEMENT OF PAVEMENT MARKING ON MEDIAN ISLANDS

GENERAL NOTES

WHEN CONCRETE CORRUGATED MEDIAN IS CONSTRUCTED TO SEPARATE TRAFFIC OPERATING IN THE OPPOSING DIRECTION, YELLOW PAVEMENT MARKING SHALL BE APPLIED TO THE FLAT PORTION OF THE CONCRETE CORRUGATED MEDIAN. THE ITEM OF PAVEMENT MARKING, CONCRETE CORRUGATED MEDIAN, WILL BE MEASURED IN PLACE AND ACCEPTED IN ACCORDANCE WITH THE CONTRACT AND PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT.

(1) APPLY PAVEMENT MARKING TO THE FLAT PORTION OF CORRUGATED MEDIAN.

ISLAND NOSE MARKING

CURB MARKING

CORRUGATED MEDIAN MARKING

DIRECTION OF TRAVEL

PAVEMENT MARKINGS, MEDIAN ISLAND NOSE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

May 2023
DATE

/S/ Jeannie Silver
STATE SIGNING AND MAR ENGINEER

FHWA

6

SDD 15C18-08b

LEGEND

V1 WORK VEHICLE

V2 SHADOW VEHICLE

TRUCK MOUNTED ATTENUATOR (TMA)

FLASHING ARROW PANEL (CAUTION)

WORK AREA

DIRECTION OF TRAFFIC

POSTED SPEED PRIOR TO WORK STARTING (MPH)	DECISION SIGHT DISTANCE (D)
0 - 25	550'
30	550'
35	700'
40	700'
45	900'
50	900'
55	1200'

GENERAL NOTES

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED.

MOBILE IS WORK THAT MOVES CONTINUOUSLY OR MOVES AT LEAST THE DECISION SIGHT DISTANCE EVERY 15 MINUTES.

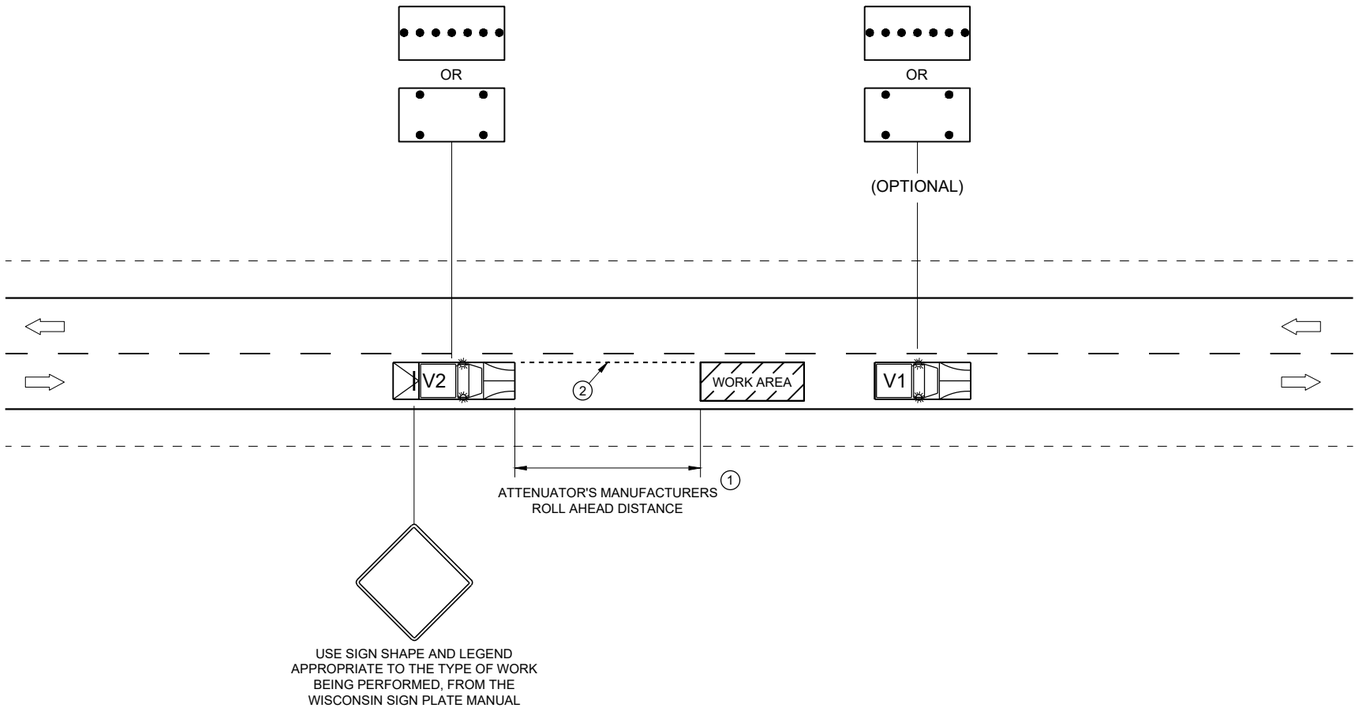
ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

ALL ARROW PANELS SHALL BE REAR FACING, TYPE "B" OR "C", AND DISPLAYING THE FLASHING CAUTION MODE. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

USE AN ATTENUATOR ON THE REARMOST VEHICLE THAT BLOCKS ALL OR PART OF THE TRAFFIC LANE.

(1) DISTANCE BETWEEN VEHICLES MAY INCREASE FROM THE ATTENUATOR'S ROLL AHEAD BASED ON TERRAIN, SIGHT DISTANCE, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.

(2) ALIGN LEFT SIDE OF SHADOW VEHICLE WITH EDGE OF WORK AREA.



TRAFFIC CONTROL, MOBILE OPERATIONS ON AN UNDIVIDED ROADWAY

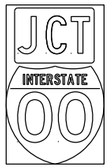
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2021 — DATE
/S/ Andrew Heidtke — STATEWIDE WORK ZONE T SAFETY ENGINEER
FHWA

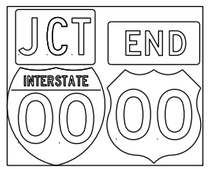
6

SDD 15D51 - 01

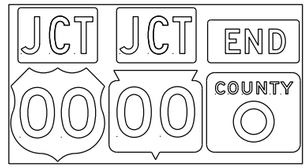
TYPICAL ASSEMBLIES



J1-1



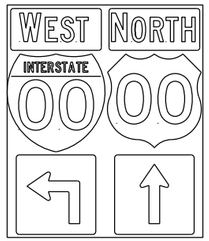
J1-2



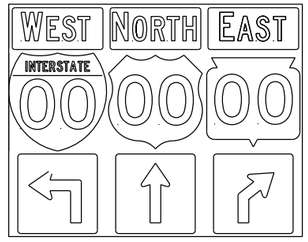
J1-3

JR1-1

J2-1

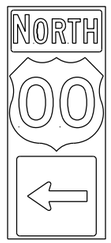


J2-2

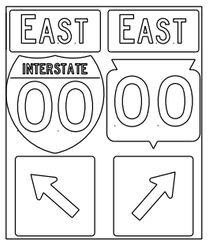


J2-3

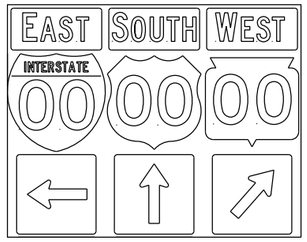
JV
(Typical Vertical J-Assembly
See Note 10 and 11)



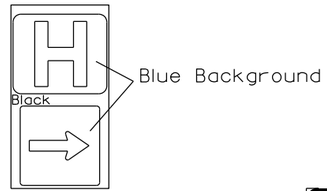
J3-1



J3-2

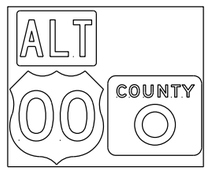


J3-3

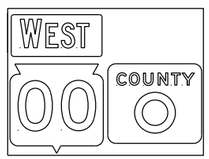


JH-1

J4-1

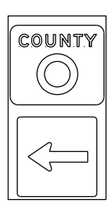


J4-2



J4-2

J12-1



J13-1

J32-1

J33-1

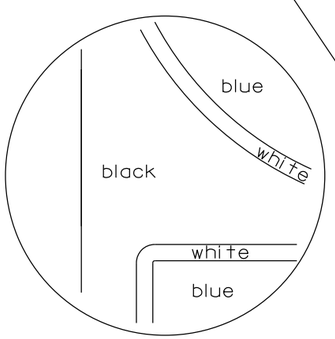
J22-1

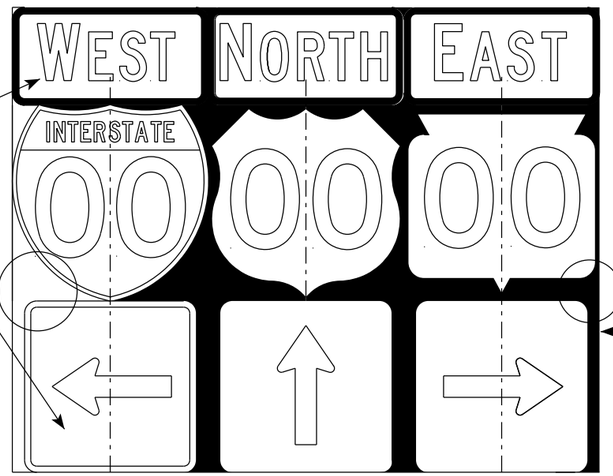
J23-1

JR13-1

JR23-1

JR99-1





NOTES

1. Signs are Type II - Type H Reflective
2. Color:
   Background - Black Non-reflective
   Message - see Note 5
3. Message Series - See Note 5
4. Corners shall be square or rounded if base material is plywood. If base material is metal the corners shall be rounded.
5. The colors and message spacing on each marker shall be according to the applicable route marker panel specifications.
6. Certain marker heads require the component pieces to be the same color. As an example, all the components used with an M1-1 Interstate marker shall be blue.
7. Single panel j-assemblies shall only be used with route marker shields that are same size. If the route marker shields are different size use multiple piece component.
8. Route assemblies that have 24 inch route shields and have dimensions greater than 48 inches (both vertical and horizontal) shall have one horizontal splice between the arrows and route shields. Vertical splices shall not be used on route assemblies with a horizontal dimension of 144 inches or less. The contractor shall not use more than one vertical joint per sign and the joint shall be between route shields.
9. Route assemblies that have 36 inch shields and have dimensions greater than 48 inchs (both vertical and horizontal) shall have two horizontal splices. One horizontal splice shall be between the cardinal direction and route shields and the other horizontal splice shall be between the arrows and route shields. Vertical splices shall not be used on route assemblies with a horizontal dimension of 144 or less. The contractor shall not use more than one vertical joint per sign and the joint shall be between route shields.
10. All Vertical J Assemblies are given a Sign Code of JV
11. For JV Assemblies that have a mixture of Interstate and non Interstate shields, arrows and cardinals shall be white on blue.

ROUTE MARKERS & COMPONENTS
IN TYPICAL ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/18/21 PLATE NO. A2-1S.9

PROJECT NO:

SHEET NO: 132 E

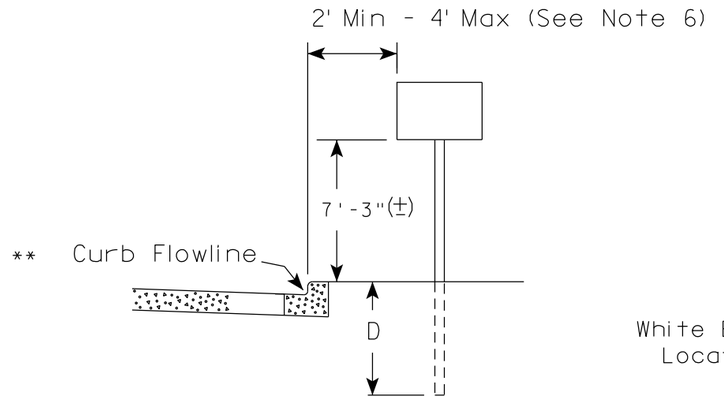
FILE NAME : C:\CAEfiles\Projects\tr_stdplate_A21S.dgn PLOT DATE : 18-MAR 2021 1:37 PLOT BY : mscj9h PLOT NAME :

WISDOT/CADDS SHEET 42

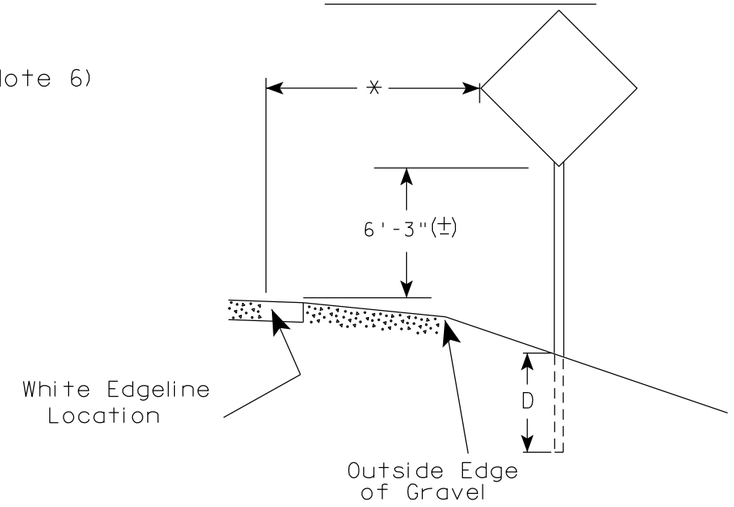
7

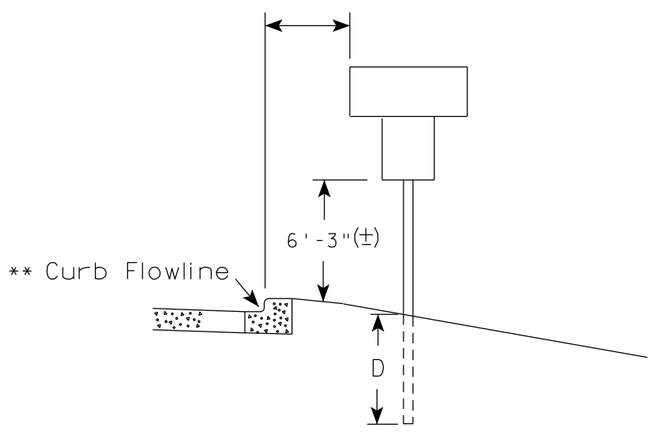
7

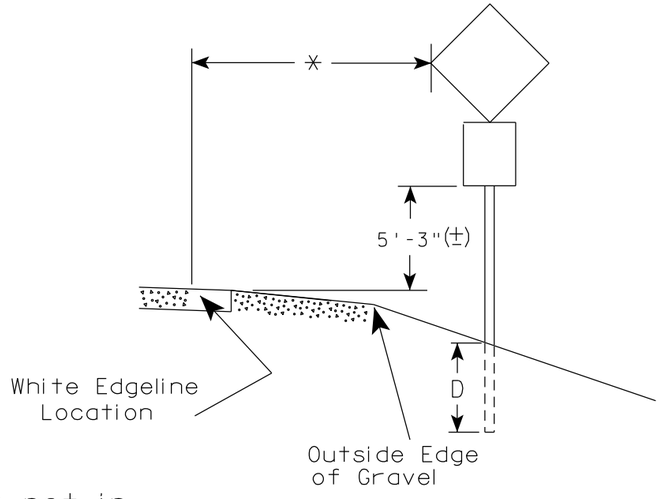
URBAN AREA



RURAL AREA (See Note 2)







** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq.Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

GENERAL NOTES

1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on or behind barrier wall, see A4-10 sign plate.
   The Double Arrow sign (W12-1D) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).
3. For expressways and freeways, mounting height is 7'- 3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
4. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±).
5. Offset distance shall be consistent with existing signs or consistent throughout length of project.
6. The (±) tolerance for mounting height is 3 inches.
7. Folding signs shall be mounted at a height of 5'-3" (±) or as directd by the Engineer.

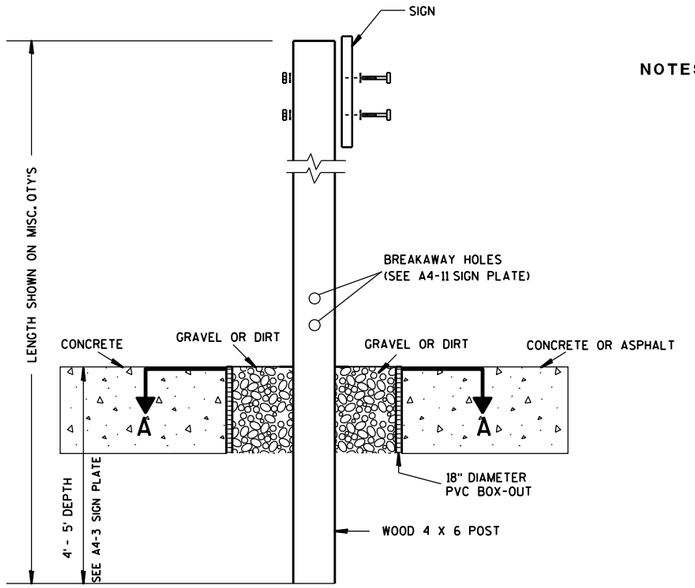
TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 5/13/2020 PLATE NO. A4-3.22

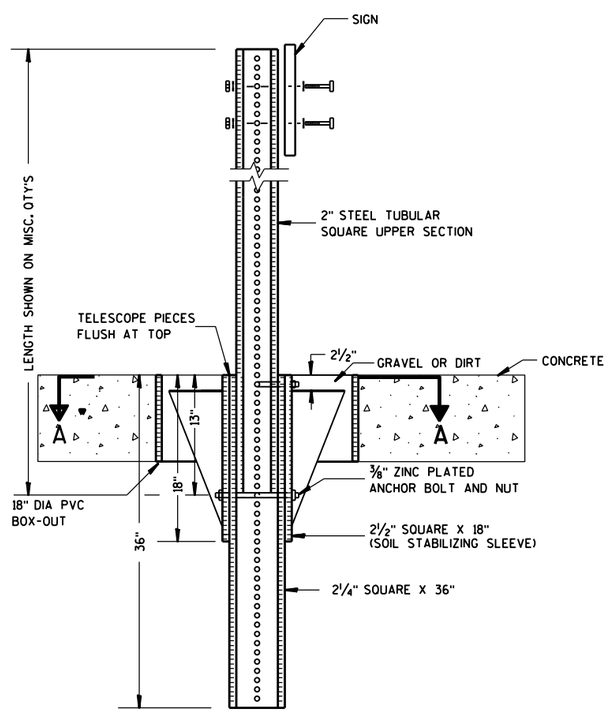
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 133	E



ELEVATION VIEW

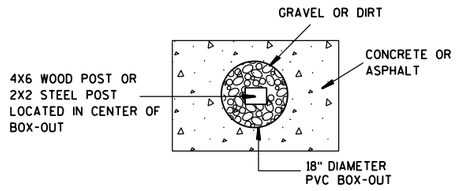
DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

NOTES:
1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



PLAN VIEW

FOR NEW CONCRETE/ASPHALT INSTALLATIONS

SIGN POST BOX-OUTS A4-3B

WISCONSIN DEPT OF TRANSPORTATION

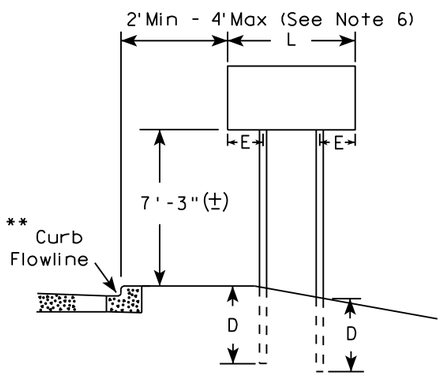
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 1/27/14 PLATE NO. A4-3B.1

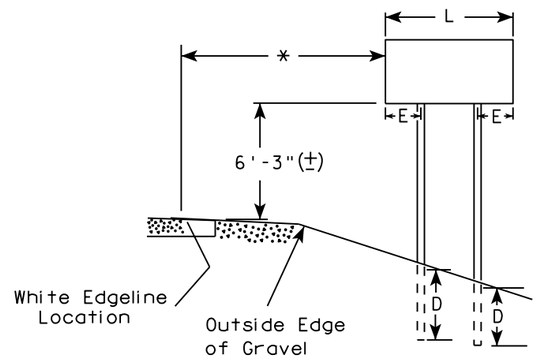
PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E

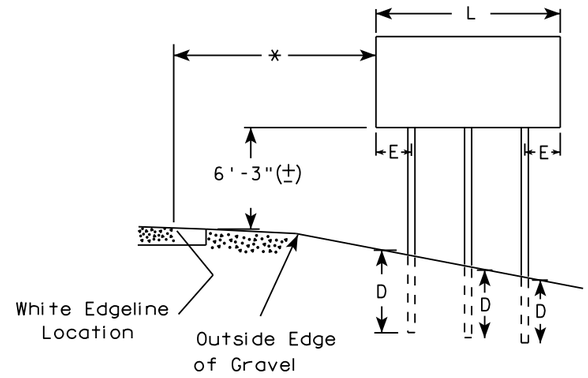
FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A43B.DGN PLOT DATE : 27-JAN-2014 09:48 PLOT BY : mscsja PLOT NAME : PLOT SCALE : 13.659812:1.000000 WISDOT/CADDS SHEET 42

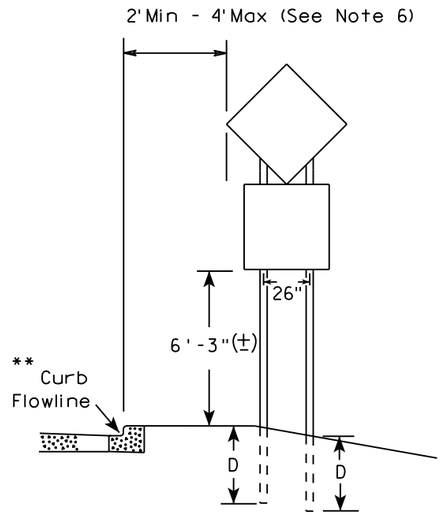
URBAN AREA



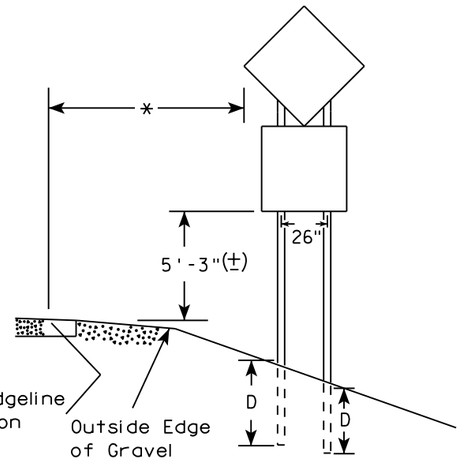
RURAL AREA (See Note 3)







48" DIAMOND WARNING SIGN



48" DIAMOND WARNING SIGN

SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)		
	L	E
***	Greater than 48" Less than 60"	12"
	60" to 108"	L/5

SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)	
L	E
Greater than 108" to 144"	12"

GENERAL NOTES

1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
2. See tables below for required number of posts.
3. For expressways and freeways, mounting height is 7'-3" (±) or 6'-3" (±) depending upon existence of sub-sign.
4. The (±) tolerance for mounting height is 3 inches.
5. J-Assemblies are considered to be one sign for mounting height.
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the engineer.
8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4"-3" (±).

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

*** See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

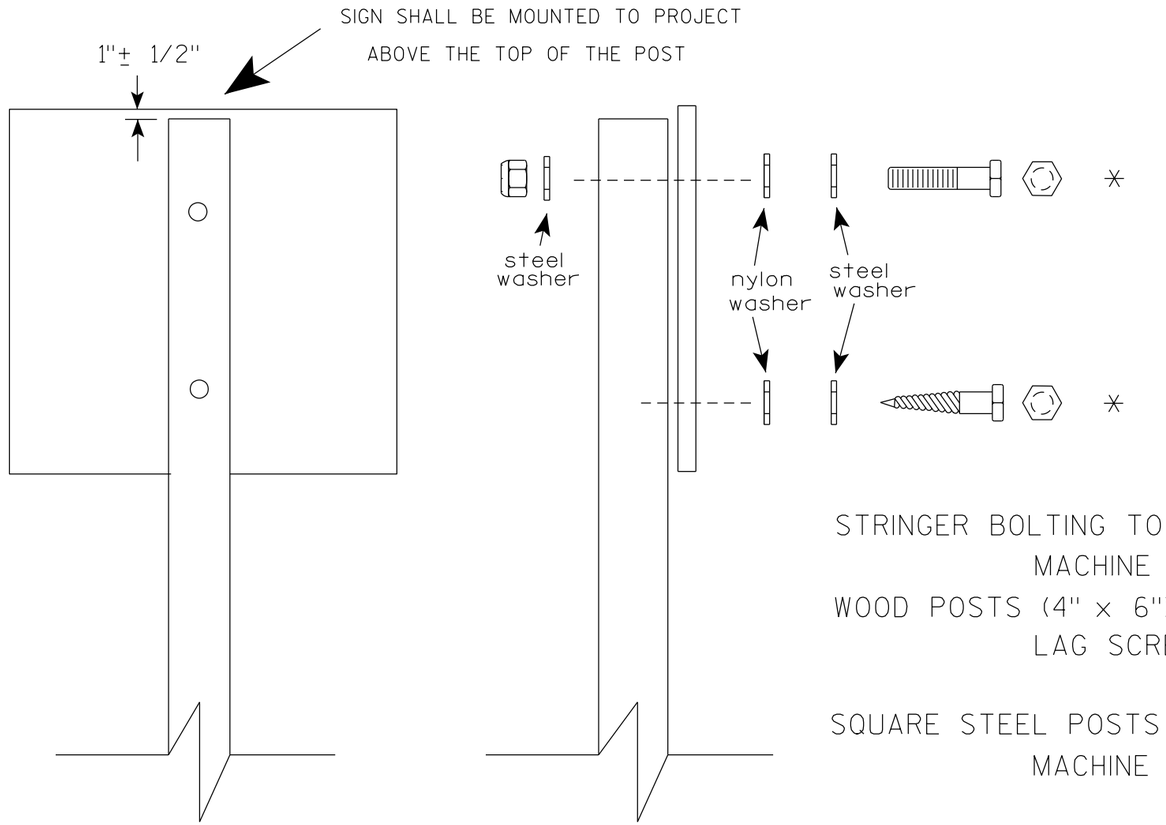
TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
For State Traffic Engineer

DATE 8/21/17 PLATE NO. A4-4.15

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 135	E



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3

b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)

MACHINE BOLTS - 5/16" X 1-3/4" Length w/ lock nuts

WOOD POSTS (4" x 6")

LAG SCREWS - 3/8" X 3" (NO STRINGERS ON BACK OF SIGN)
3/8" X 4" (STRINGERS ON BACK OF SIGN)

SQUARE STEEL POSTS (2" x 2")

MACHINE BOLTS - 3/8" X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)
3/8" X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)

RIVETS - 9/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH

WASHERS (ALL POSTS) -

1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
1-1/4" O.D. X 3/8" I.D. X .080 NYLON

* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS TO POSTS

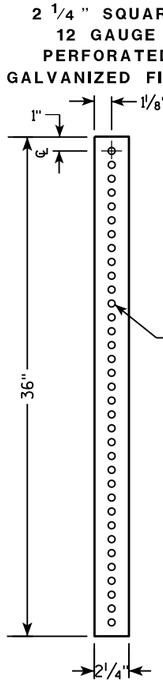
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

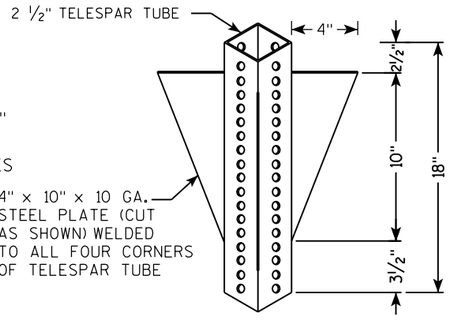
DATE 4/1/2020 PLATE NO. A4-8.9

TELESCOPIC TUBING ANCHORS
TWO PIECE SYSTEM

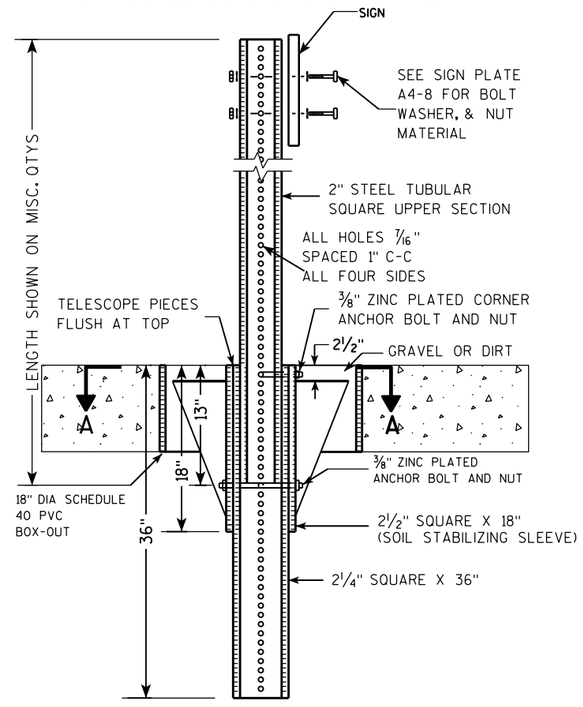
2 1/4 " SQUARE
12 GAUGE
PERFORATED
GALVANIZED FINISH



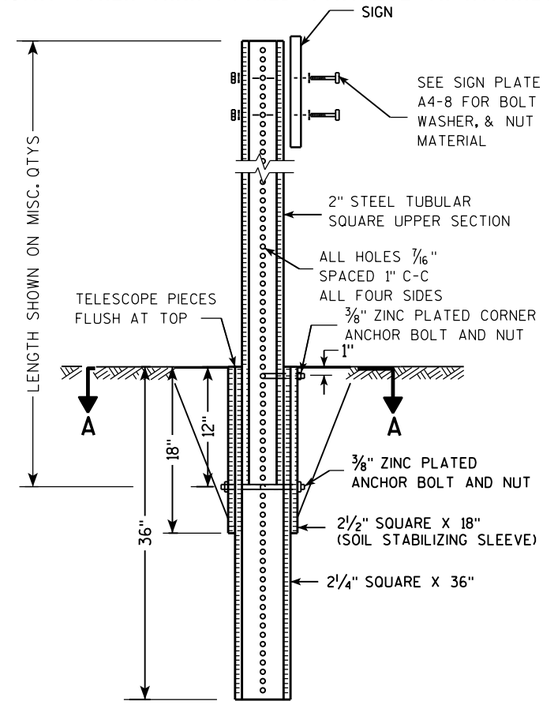
2 1/2 " SQUARE
12 GAUGE
OMNI-DIRECTIONAL
PERFORATED
SOIL STABILIZING SLEEVE
GALVANIZED FINISH

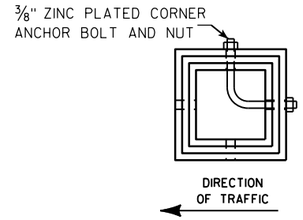


DETAIL OF TUBULAR STEEL SIGN POST
(IN POURED CONCRETE OR ASPHALT)



DETAIL OF TUBULAR STEEL SIGN POST
(IN LOCATIONS OTHER THAN POURED CONCRETE OR ASPHALT)





SECTION A-A

Area of Sign Installation (Sq. Ft.)	Number of Required Posts
9 or less	1
Greater than 9 less than or equal to 18	2
Greater than 18 less than or equal to 27	3

Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

TUBULAR STEEL
SIGN POST
A4-9

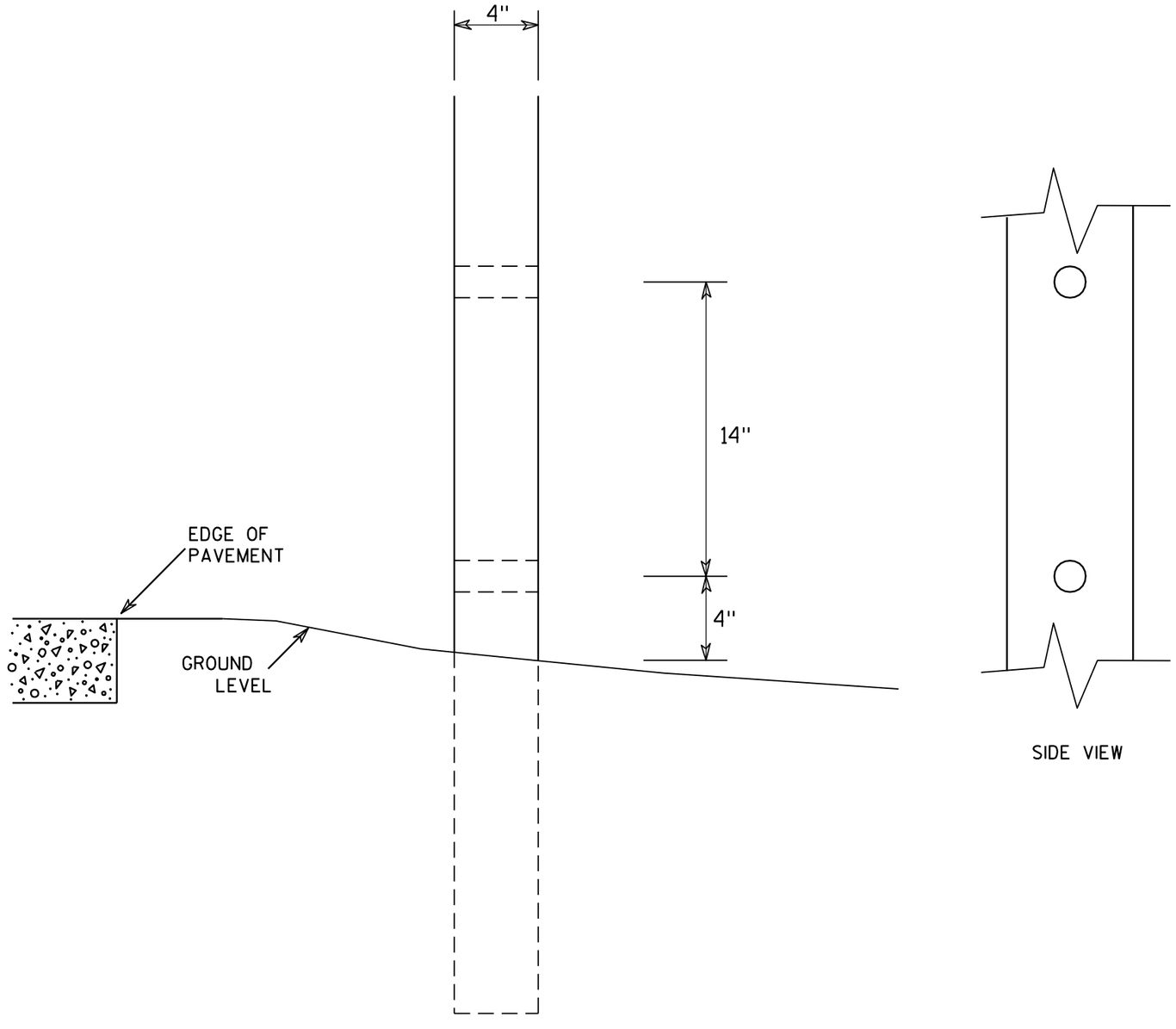
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 2/05/15 PLATE A4-9.9

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A49.DGN PLOT DATE : 05-FEB-2015 17:09 PLOT BY : mscsja PLOT NAME : PLOT SCALE : 13.659812:1.000000 WISDOT/CADDS SHEET 42



SIDE VIEW

GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

7

7

4 X 6 WOOD POST MODIFICATIONS

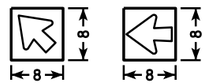
WISCONSIN DEPT OF TRANSPORTATION

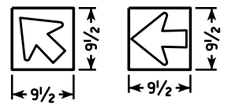
APPROVED Chester J Spang for State Traffic Engineer

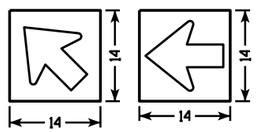
DATE 3/27/97 PLATE NO. A4-11.2

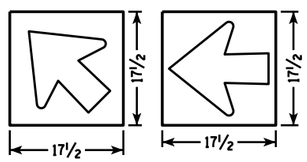
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 138	E

SIGN LAYOUT WITH VARIOUS SIZED MESSAGES







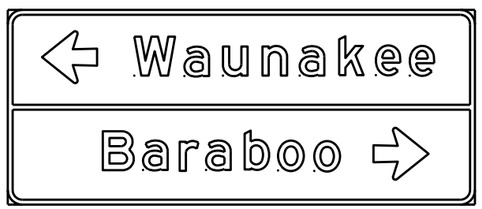


GENERAL NOTES

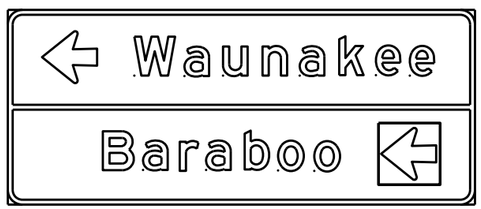
1. Materials shall conform to Standard Specification Section 637.
   Base - Sheet Aluminum 0.040" Thickness
   Sheeting - Orange Type F Reflective
   Arrow - Black Non-Reflective
2. Arrow signs shall be fastened to permanent sign by either aluminum rivets or aluminum self-tapping sheet metal screws. There shall be a minmum of 2 fasteners used per arrow sign.
3. There shall be a spacer consisting of a 0.08" nylon washer between the back of the arrow sign and the face of the permanent sign.
4. Arrows are per standard plate A1-2
5. Use separate arrow sign for each destination
6. Tilt arrow is always at 45 degrees
7. Arrow is centered on arrow sign

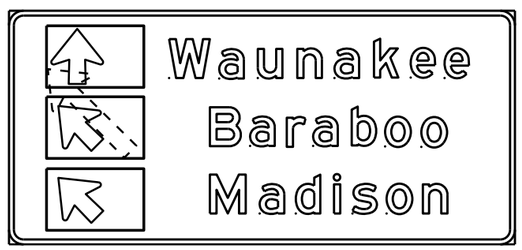
Lower Case Copy Size	Standard Width (Single Arrow)	2 Line Tilt Arrow Cover Width	3 Line Tilt Arrow Cover Width	Height
3¾" Series C	8	9 ½	14 ½	8
4½" Series D & E	9 ½	10	15	9 ½
6" Series D & E	14	16	20 ½	14
8" Series E	17 ½	20 ½	25	17 ½

BEFORE



AFTER





DESTINATION DIRECTIONAL ARROW FOR DETOUR SIGNS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 10/08/14

A4-12.2

PROJECT NO:

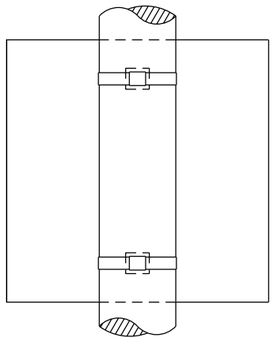
SHEET NO: E

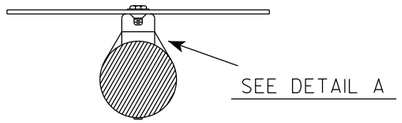
FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A412.DGN PLOT DATE : 08-OCT-2014 11:50 PLOT BY : mscsja

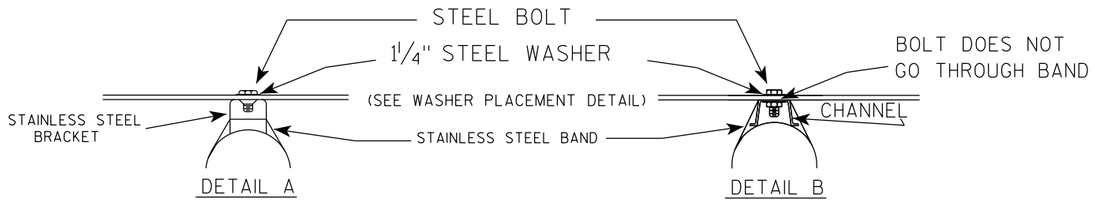
WISDOT/CADDS SHEET 42

BANDING

SINGLE SIGN



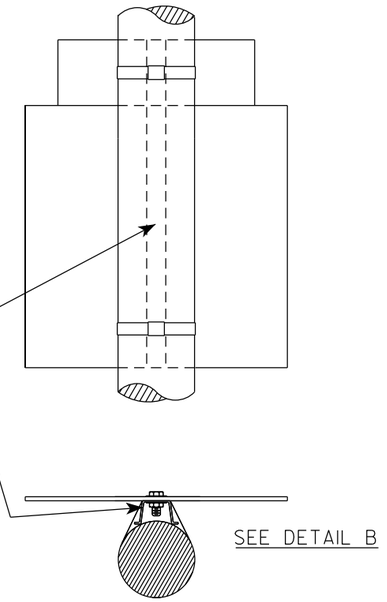




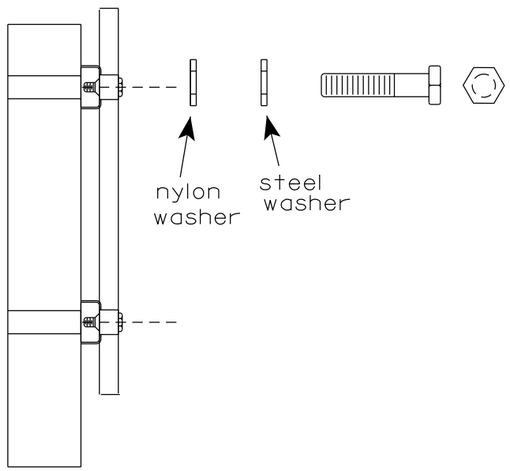
GENERAL NOTES

1. Any sign over 3 feet in width shall use the V-Block banding method. See A5-10 standard plate.
2. Signs 3 feet or greater in height shall have three bracket bands installed. Signs less than 3 feet in height shall have two bracket bands installed.
3. Banding and assembly bracket shall be stainless steel. All bands shall be ¾" in width and 0.025" thickness.
4. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
   a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
   b. Electro-galvanized in accordance with ASTM designation: B 633, Type III, SC 3

"J" ASSEMBLY



WASHER PLACEMENT



WASHERS (ALL POSTS) -
1-1/4" O.D. X ⅜" I.D. X 1/16" STEEL
1-1/4" O.D. X ⅜" I.D. X .080 NYLON
FOR ALL TYPE H SIGNS

STANDARD SIGN
SIGN BANDING DETAILS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

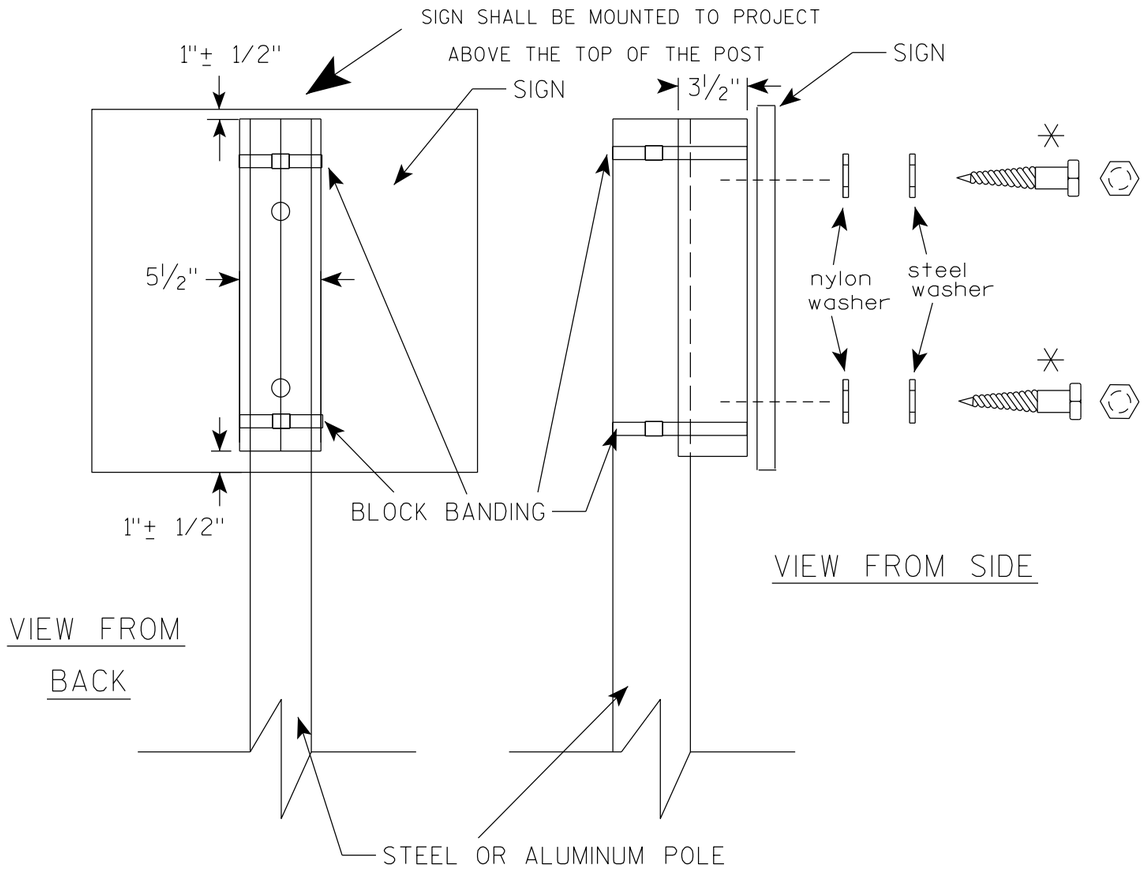
DATE 6/10/19 PLATE NO. A5-9.4

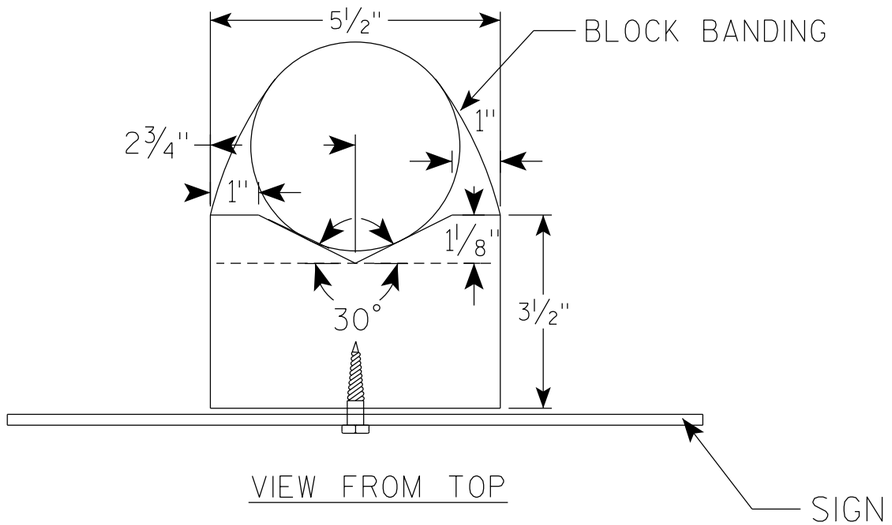
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 140	E

FILE NAME : C:\CAEfiles\Projects\tr_stdplate\A59.dgn PLOT DATE : 10-JUN 2019 4:10 PLOT BY : mscj9h PLOT NAME : PLOT SCALE : $$.....plotscale.....$$ WISDOT/CADDS SHEET 42

7

7





GENERAL NOTES

1. WOOD 4"X6" POST MATERIAL SHALL CONFORM TO 507.2.2 OF THE WisDOT STANDARD SPECIFICATIONS
2. BLOCK BANDING AND CLIPS SHALL BE STAINLESS STEEL, ¾" WIDTH AND 0.025" THICKNESS
3. SIGNS 3' OR GREATER IN HEIGHT SHALL UTILIZE 3 BLOCK BANDS. SIGNS UNDER 3' IN HEIGHT SHALL UTILIZE 2 BLOCK BANDS
4. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA, BUT NORNALLY THERE ARE TWO. FOR SIGNS GREATER THAN 9 S.F. 3 FASTENERS SHALL BE USED.
5. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
   a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
   b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3
6. ALL BOLTS SHALL HAVE HEXAGONAL HEADS.
7. STEEL WASHERS SHALL BE 1¼" O.D. X ⅜" I.D. X 1/16"
8. NYLON WASHERS SHALL BE 1¼" O.D. X ⅜" I.D. X .080 FOR TYPE H OR TYPE F FACE SIGN

✳ LAG BOLTS SHALL BE ⅜" X 2½"

BLOCK BANDING DETAIL (V-BLOCK OPTION)

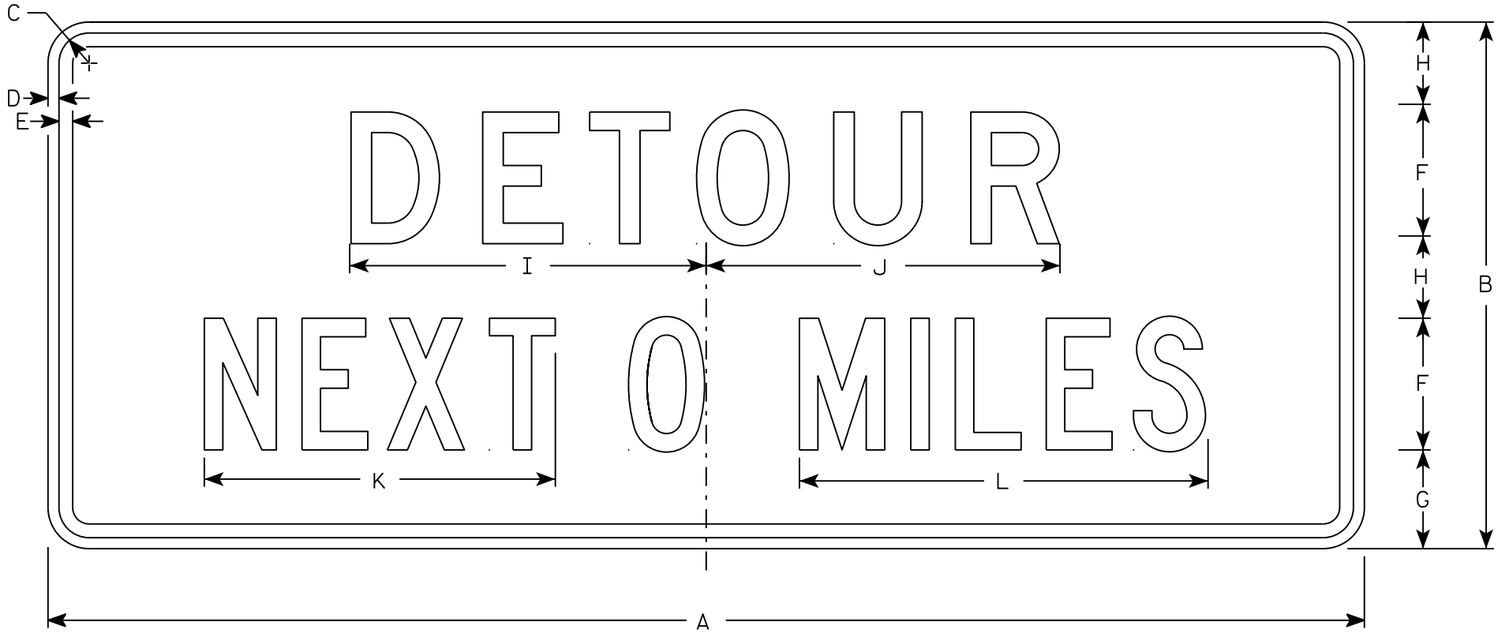
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 4/19/2022 PLATE NO. A5-10.3

PROJECT NO:

SHEET NO: 141 E



G20-51

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
   Background - Orange
   Message - Black
3. Message Series - Line 1 is D and Line 2 is C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Round distance to nearest whole Mile and substitute appropriate numerals and optically adjust spacing to achieve proper balance

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	60	24	1 3/8	1/2	5/8	6	4 1/2	3 3/4	16 1/4	16 1/8	16	18 5/8															10
3																											
4	60	24	1 3/8	1/2	5/8	6	4 1/2	3 3/4	16 1/4	16 1/8	16	18 5/8															10
5																											

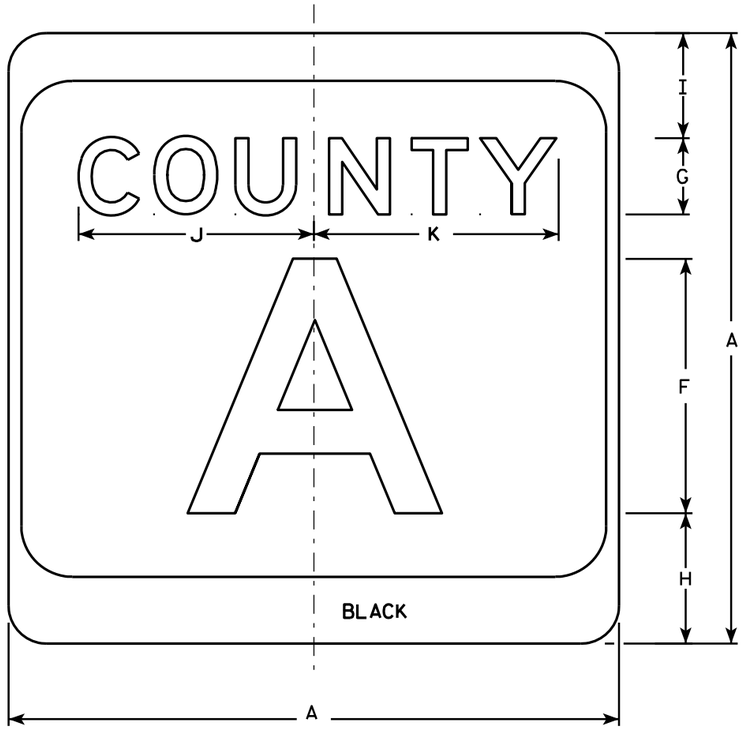
STANDARD SIGN
G20-51

WISCONSIN DEPT OF TRANSPORTATION

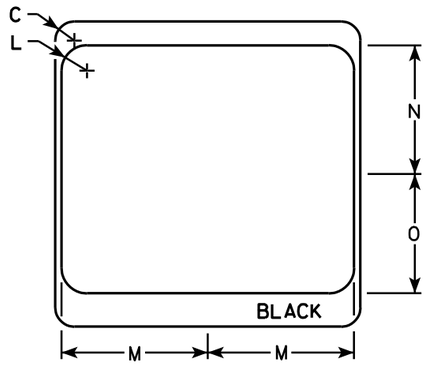
APPROVED Matthew R Rauch
for State Traffic Engineer

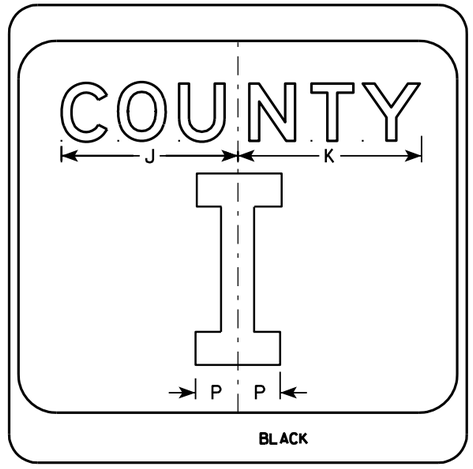
DATE 3/14/17 PLATE NO. G20-51.2

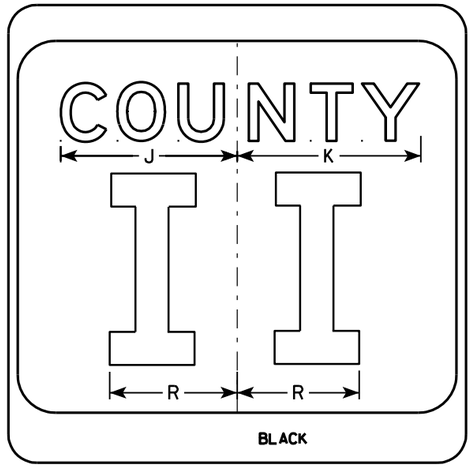
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 142	E



M1-5A







NOTES

1. Sign is Type II - see Note 7 - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White & Black - See Note 7
   Message - Black
3. Message Series - see Note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Message Series E for 1 letter.
   Message Series D for 2 letters unless message is too big then Series C.
   Message Series C for 3 letters unless message is too big then Series B.
6. Substitute appropriate letters & optically center to achieve proper balance.
7. Permanent Signs
   Background - Type H Reflective
   Detour or temporary Signs
   Background - Reflective

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24		1 1/2			10	3	5 1/8	4 1/8	9 1/4	9 5/8	2	11 1/2	10 1/8	9 3/8	2 1/4		6 5/8									4.0
3	36		2 1/4			16	4	7 5/8	5 5/8	12 1/4	12 7/8	3	17 1/8	15 1/4	14	3 3/8		10									9.0
4	36		2 1/4			16	4	7 5/8	5 5/8	12 1/4	12 7/8	3	17 1/8	15 1/4	14	3 3/8		10									9.0
5	36		2 1/4			16	4	7 5/8	5 5/8	12 1/4	12 7/8	3	17 1/8	15 1/4	14	3 3/8		10									9.0

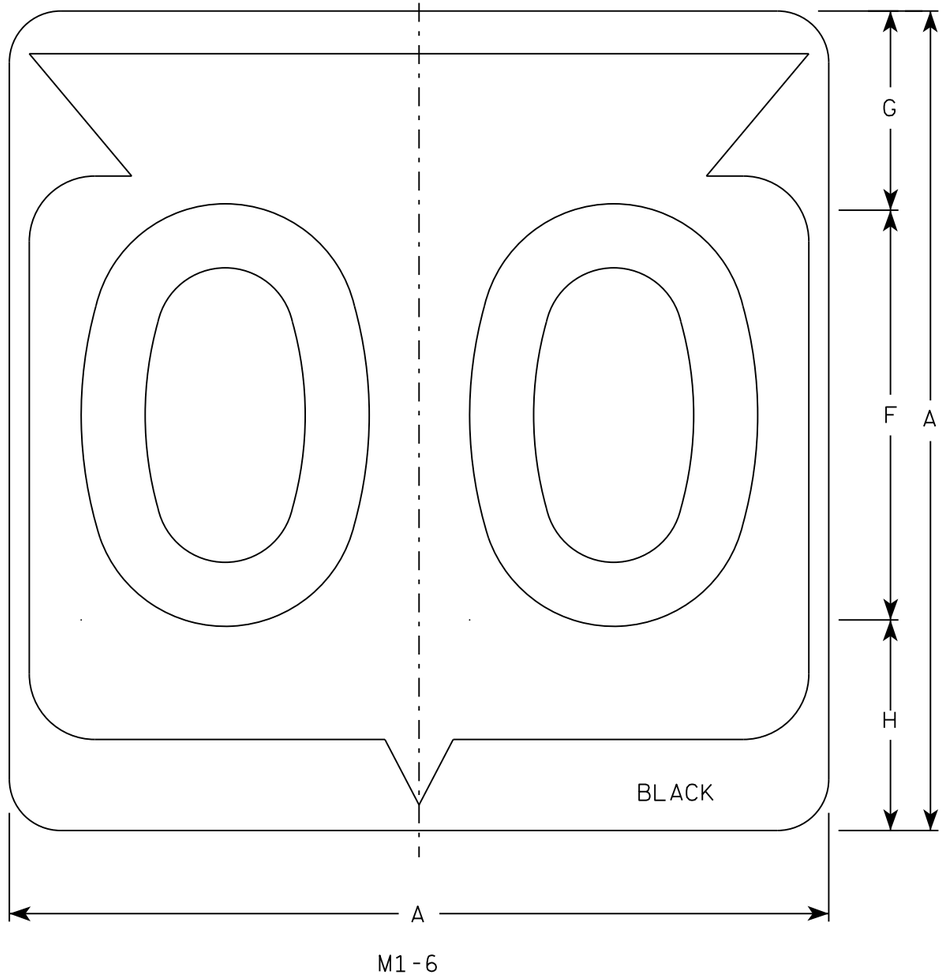
CTH MARKER
M1-5A FOR ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 9/27/11 PLATE NO. M1-5A.8

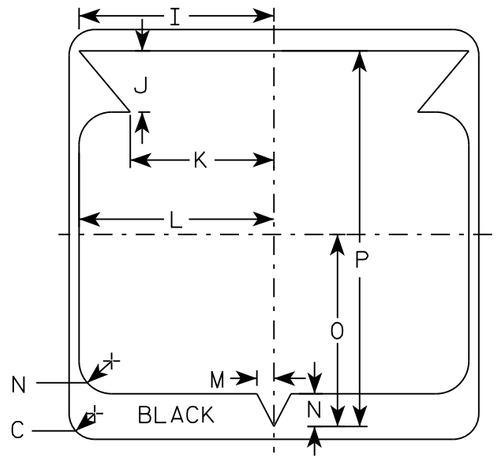
PROJECT NO: | HWY: | COUNTY: | SHEET NO: 143 | E



M1-6

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
   Background - White
   Message - Black
3. Message Series - D except 3 number signs Series C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24		1 1/2			12	5 1/2	6 1/2	10 1/4	2 1/2	8 7/8	11 1/2	1	1 7/8	11 1/4	21 7/8											4.0
3	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0
4	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0
5	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0

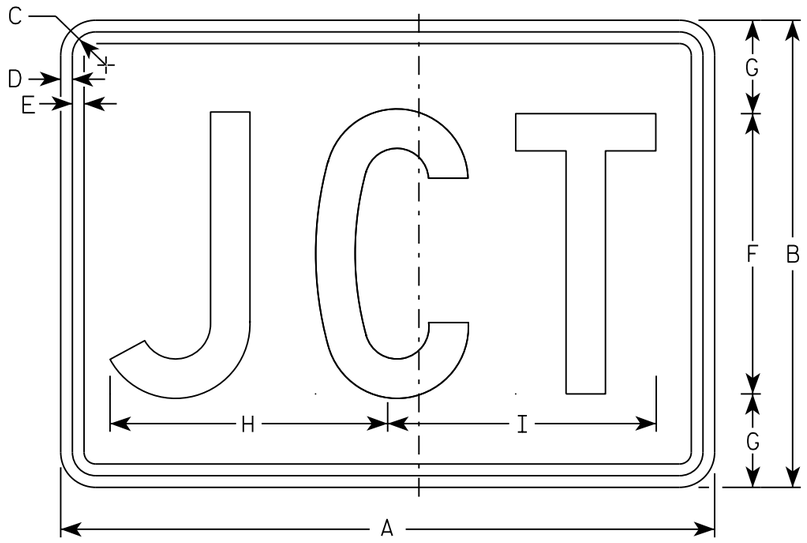
STATE ROUTE MARKER M1-6 FOR ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

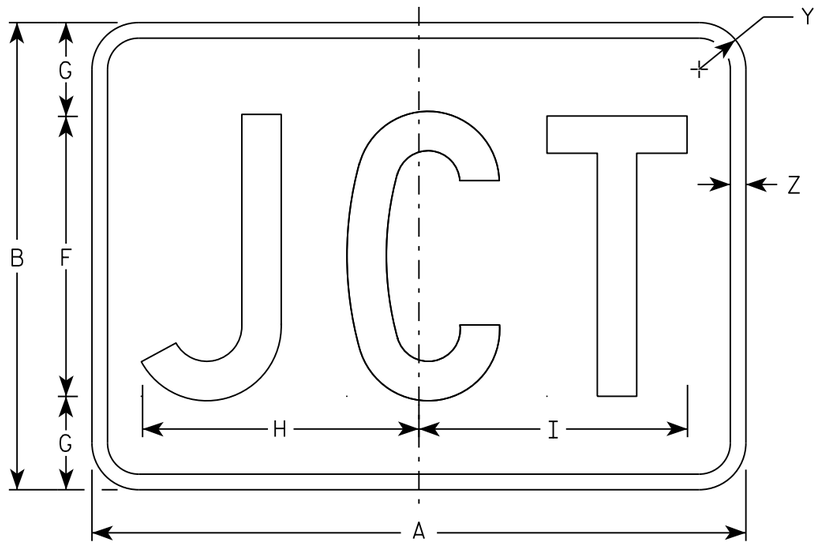
APPROVED Matthew R Rauch for State Traffic Engineer

DATE 3/16/18 PLATE NO. M1-6.10

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 144 | E



M2-1
MM2-1
MP2-1



MB2-1
MK2-1
MN2-1
MR2-1

NOTES

1. Sign is Type II - Type H
2. Color:
   Background - See note 5
   Message - See note 5
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. M2-1 Background - White
   Message - Black
   MB2-1 Background - Blue
   Message - White
   MK2-1 Background - Green
   Message - White
   MM2-1 Background - White
   Message - Green
   MN2-1 Background - Brown
   Message - White
   MP2-1 Background - White
   Message - Blue
   MR2-1 Background - Brown
   Message - Yellow

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21	15	1 1/8	3/8	3/8	9	3	8 7/8	8 5/8																1 1/2	1/2	2.20
3	30	21	1 1/8	3/8	3/8	13	4	12 7/8	12 3/8																1 1/2	1/2	4.40
4	30	21	1 1/8	3/8	3/8	13	4	12 7/8	12 3/8																1 1/2	1/2	4.40
5	30	21	1 1/8	3/8	3/8	13	4	12 7/8	12 3/8																1 1/2	1/2	4.40

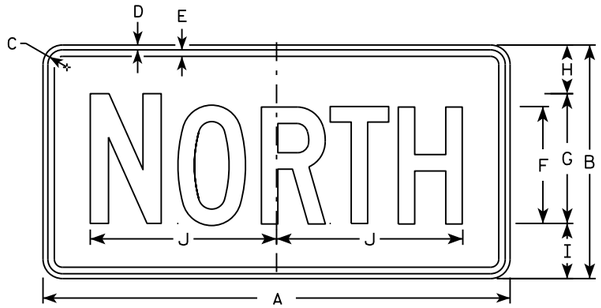
STANDARD SIGN
M2-1

WISCONSIN DEPT OF TRANSPORTATION

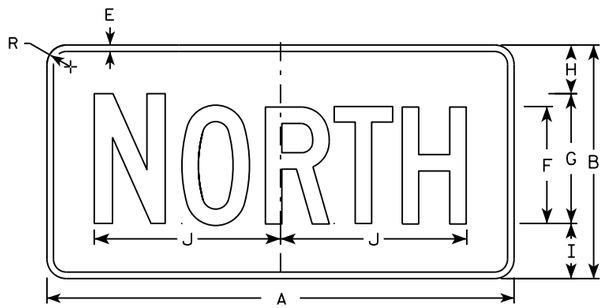
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 10/15/15 PLATE 145 12-1.12

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E



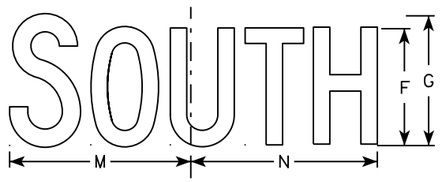
M3-1
MM3-1
MP3-1



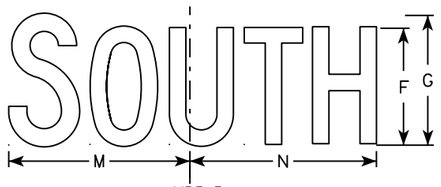
MB3-1
MK3-1
MN3-1

M3-2
MM3-2
MP3-2

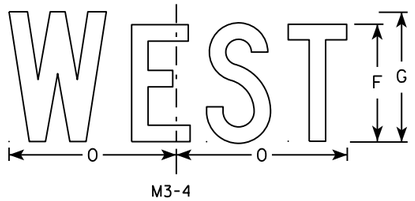
MB3-2
MK3-2
MN3-2



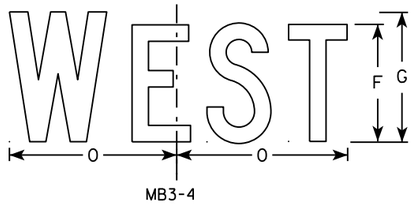
M3-3
MM3-3
MP3-3



MB3-3
MK3-3
MN3-3



M3-4
MM3-4
MP3-4



MB3-4
MK3-4
MN3-4

NOTES

1. All Signs Type II - Type H
2. Color:
   Background - See note 5
   Message - See note 5
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. M3-1 thru M3-4 Background - White
   Message - Black
   MB3-1 thru MB3-4 Background - Blue
   Message - White
   MK3-1 thru MK3-4 Background - Green
   Message - White
   MM3-1 thru MM3-4 Background - White
   Message - Green
   MN3-1 thru MN3-4 Background - Brown
   Message - White
   MP3-1 thru MP3-4 Background - White
   Message - Blue
6. Note the first letter of each direction is larger than the remainder of the message.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	12	1 1/8	3/8	3/8	6	7	2 1/4	2 3/4	10 1/4	7 7/8	8 3/8	10 1/4	9 3/4	8 3/4			1 1/2									2.00
3	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
4	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
5	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5

STANDARD SIGNS
M3-1 thur M3-4
SERIES

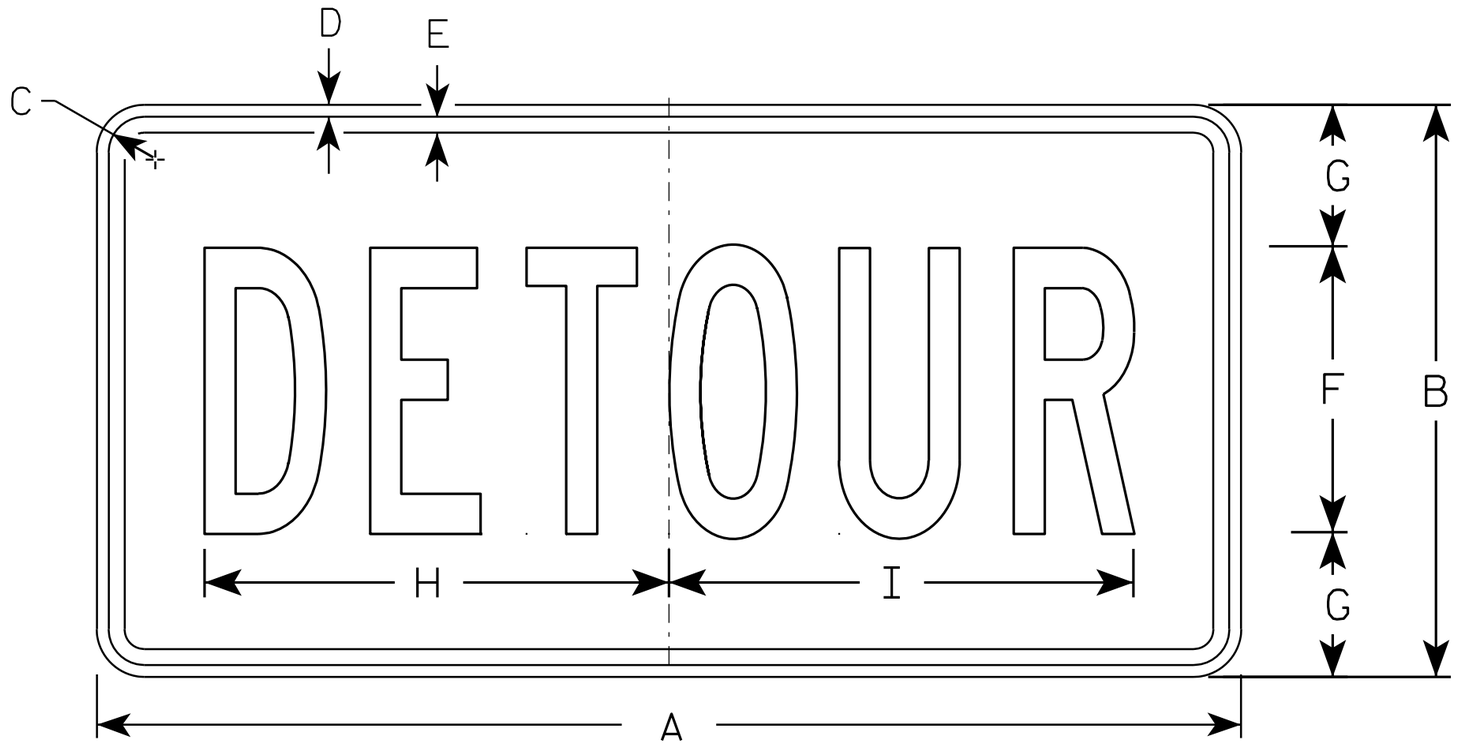
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew P Rauch for State Traffic Engineer

DATE 10/15/15 PLATE NO M3-1.14

PROJECT NO: | HWY: | COUNTY: | SHEET NO: E

FILE NAME : C:\CAEfiles\Projects\tr_stdplate\M31.DGN PLOT DATE : 01-DEC-2015 17:54 PLOT BY : $$ plotuser $$ PLOT NAME : PLOT SCALE : 11.675051:1.000000



M4-8

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Message Series - B
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	12	1 1/8	3/8	3/8	6	3	10	10 1/4																		2.0
3	36	18	1 1/8	3/8	1/2	9	4 1/2	14 5/8	14 1/2																		4.5
4																											
5																											

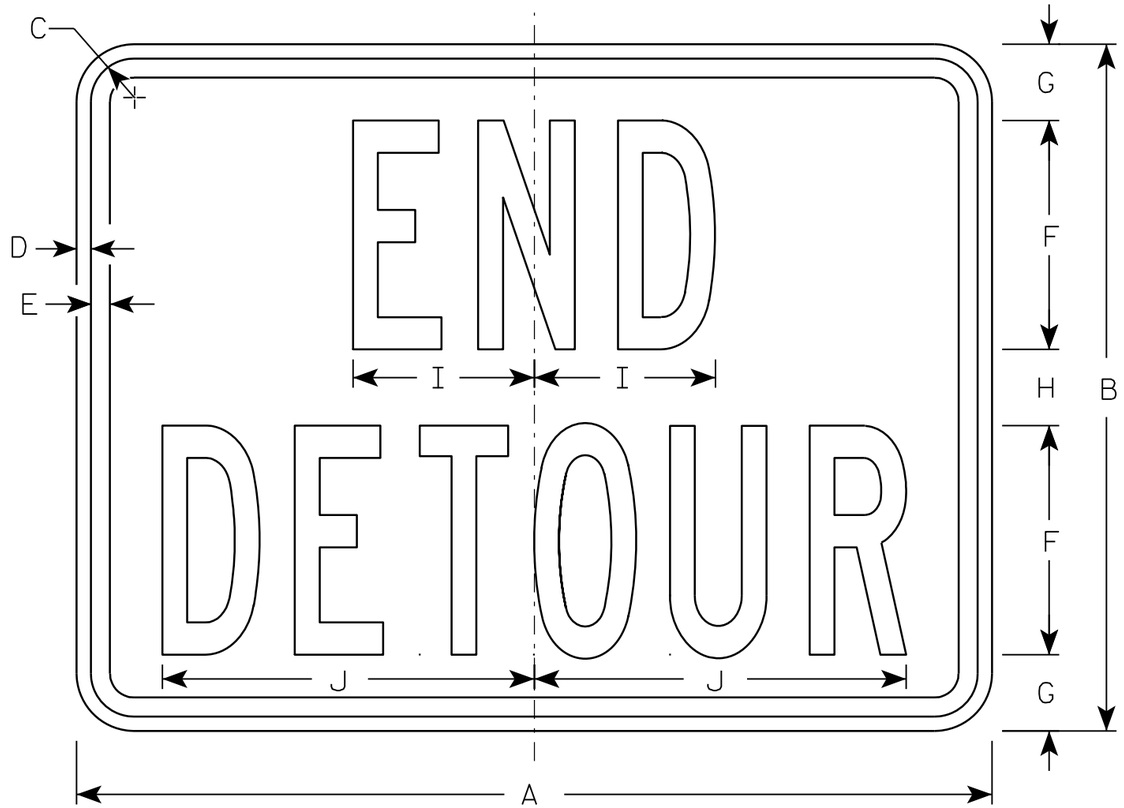
STANDARD SIGN
M4-8

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 11/10/10 PLATE NO. M4-8.2

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 147	E



M4-8A

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Message Series - B
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	18	1 1/8	3/8	1/2	6	2	2	4 3/4	9 3/4																	3.0
3	30	24	1 1/8	3/8	1/2	8	2 1/2	3	6 3/4	13																	5.0
4																											
5																											

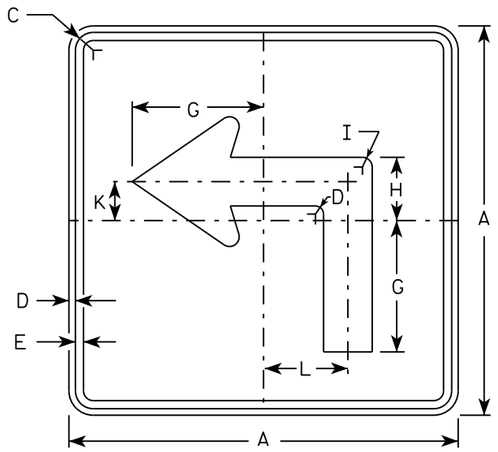
STANDARD SIGN
M4-8A

WISCONSIN DEPT OF TRANSPORTATION

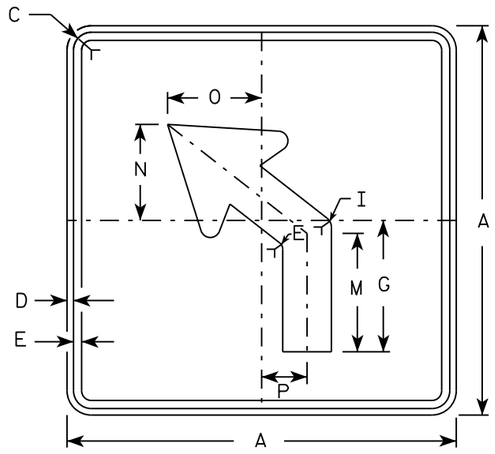
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/9/11 PLATE NO. M4-8A.2

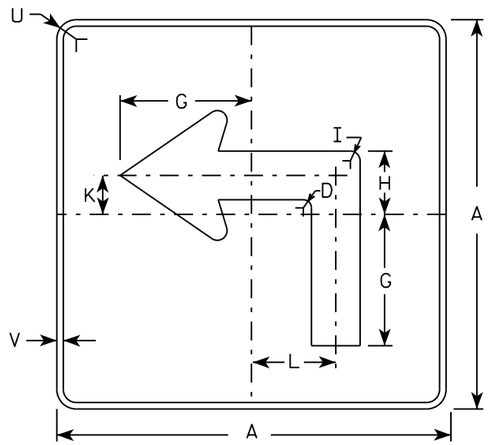
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 148	E



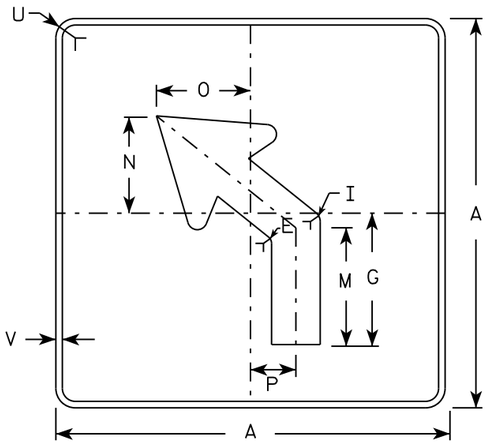
M5-1L
MM5-1L
MO5-1L
MP5-1L



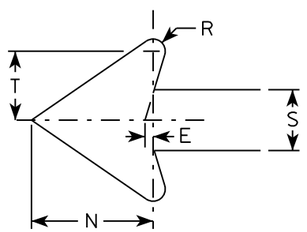
M5-2L
MM5-2L
MO5-2L
MP5-2L



MB5-1L
MK5-1L
MN5-1L
MR5-1L



MB5-2L
MK5-2L
MN5-2L
MR5-2L



NOTES

1. Signs are Type II - Type H reflective except as shown
2. Color:
   Background - See note 4
   Message - See note 4
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. M5-1 and M5-2 Background - White
   Message - Black
   MB5-1 and MB5-2 Background - Blue
   Message - White
   MK5-1 and MK5-2 Background - Green
   Message - White
   MM5-1 and MM5-2 Background - White
   Message - Green
   MN5-1 and MN5-2 Background - Brown
   Message - White
   MO5-1 and MO5-2 Background - Orange - Type F Reflective
   Message - Black
   MP5-1 and MP5-2 Background - White - Type H Reflective
   Message - Blue
   MR5-1 and MR5-2 Background - Brown
   Message - Yellow
5. M5-1R same as M5-1L except arrow points right.
6. M5-2R same as M5-2L except arrow tilts right.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7	3 3/8	5/8		2 1/8	4 1/2	6 3/8	5 1/4	5	2 1/2		1/2	2 5/8	3	1 1/2	1/2					3.06
3	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25
4	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25
5	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25

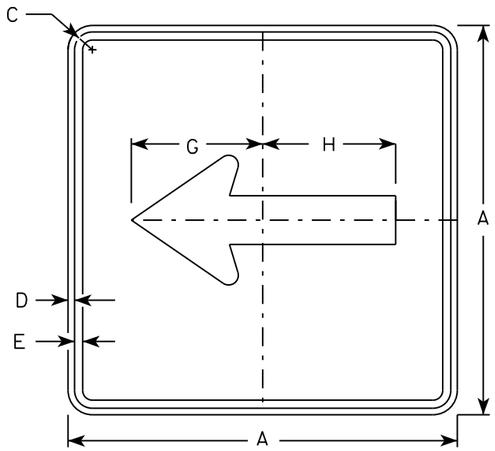
STANDARD SIGN
M5-1 & M5-2

WISCONSIN DEPT OF TRANSPORTATION

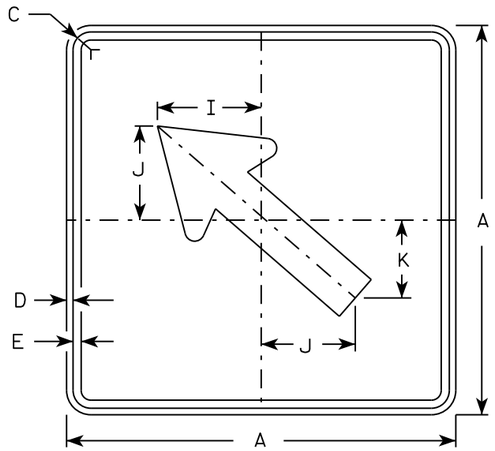
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 10/15/15 PLATE NO. M5-1.13

PROJECT NO: | HWY: | COUNTY: | SHEET NO: E



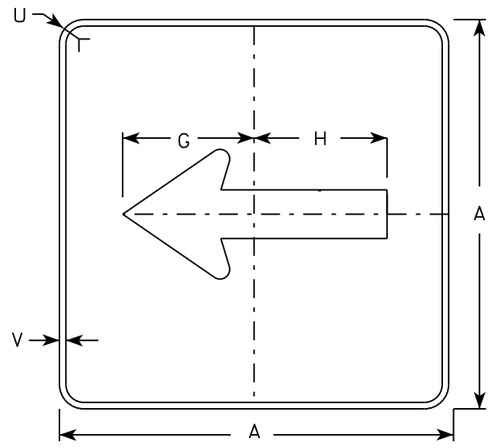
M6-1
MM6-1
MO6-1
MP6-1



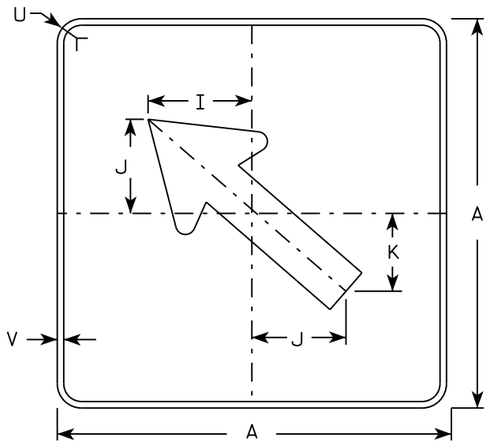
M6-2
MM6-2
MO6-2
MP6-2

NOTES

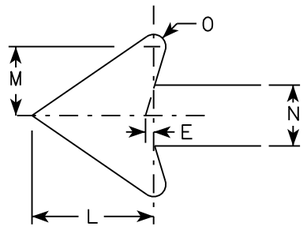
1. Signs are Type II - Type H except as Shown
2. Color:
   Background - See note 4
   Message - See note 4
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. M6-1 and M6-2 Background - White
   Message - Black
   MB6-1 and MB6-2 Background - Blue
   Message - White
   MK6-1 and MK6-2 Background - Green
   Message - White
   MM6-1 and MM6-2 Background - White
   Message - Green
   MN6-1 and MN6-2 Background - Brown
   Message - White
   MO6-1 and MO6-2 Background - Orange - Type F Reflective
   Message - Black
   MP6-1 and MP6-2 Background - White
   Message - Blue
   MR6-1 and MR6-2 Background - Brown
   Message - Yellow



MB6-1
MK6-1
MN6-1
MR6-1



MB6-2
MK6-2
MN6-2
MR6-2



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7 1/2	7 1/8	5 5/8	5	4 1/4	5 1/4	3	2 5/8	1/2						1 1/2	1/2						3.06
3	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2						6.25
4	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2						6.25
5	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2						6.25

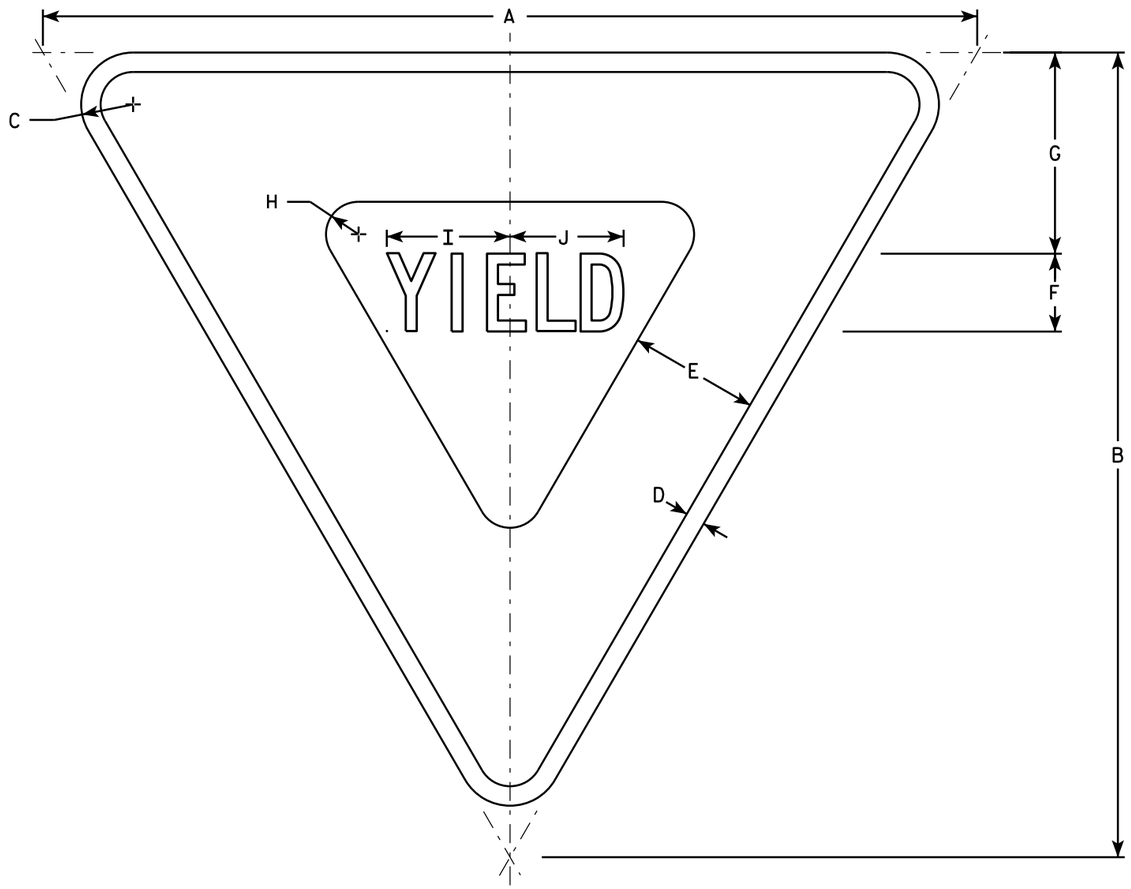
STANDARD SIGN M6-1 & M6-2 SERIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 10/15/15 PLATE 150 M6-1.15

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E



R1-2

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White
   Message - See note 5
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. The border strip and word message are reflectorized red.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30	26	1 1/2	5/8	4	2 1/2	6 3/8	7/8	4	3 5/8																	2.71
2S	36	31	2	3/4	5	3	7 3/4	1 1/4	4 3/4	4 3/8																	3.88
2M	48	42	3	1	6	4	9 3/4	2	6 1/4	5 7/8																	7.00
3	48	42	3	1	6	4	9 3/4	2	6 1/4	5 7/8																	7.00
4	48	42	3	1	6	4	9 3/4	2	6 1/4	5 7/8																	7.00
5	60	52	3	1 1/2	8	5	13	2 1/2	7 7/8	7 1/4																	10.83
6																											
7	18	15 1/2	1	3/8	2 1/2	1 1/2	3 7/8	5/8	2 3/8	2 1/4																	0.97

STANDARD SIGN
R1-2

WISCONSIN DEPT OF TRANSPORTATION

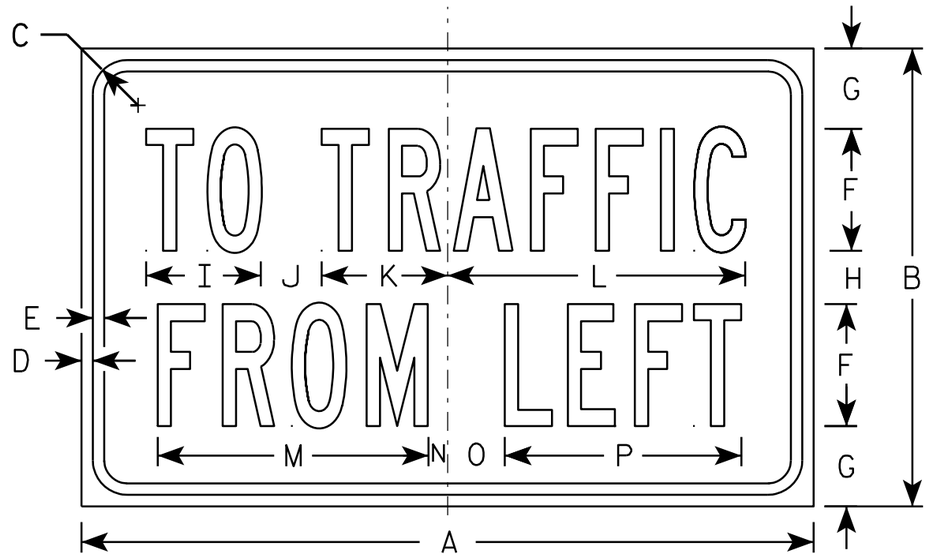
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 10/13/14 PLATE R1-2.12

PROJECT NO: | HWY: | COUNTY: | | SHEET NO: E

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\R12.DGN PLOT DATE : 13-OCT-2014 15:12 PLOT BY : mscsja PLOT NAME : PLOT SCALE : 5.837526:1.000000

WISDOT/CADDS SHEET 42



R1-54

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White
   Message - Black
3. Message Series - B

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	15	1 1/8	3/8	3/8	4	2 5/8	1 3/4	3 3/4	2	4 1/8	9 3/4	8 7/8	5/8	1 7/8	7 3/4											2.5
2M	24	15	1 1/8	3/8	3/8	4	2 5/8	1 3/4	3 3/4	2	4 1/8	9 3/4	8 7/8	5/8	1 7/8	7 3/4											2.5
3																											
4																											
5																											

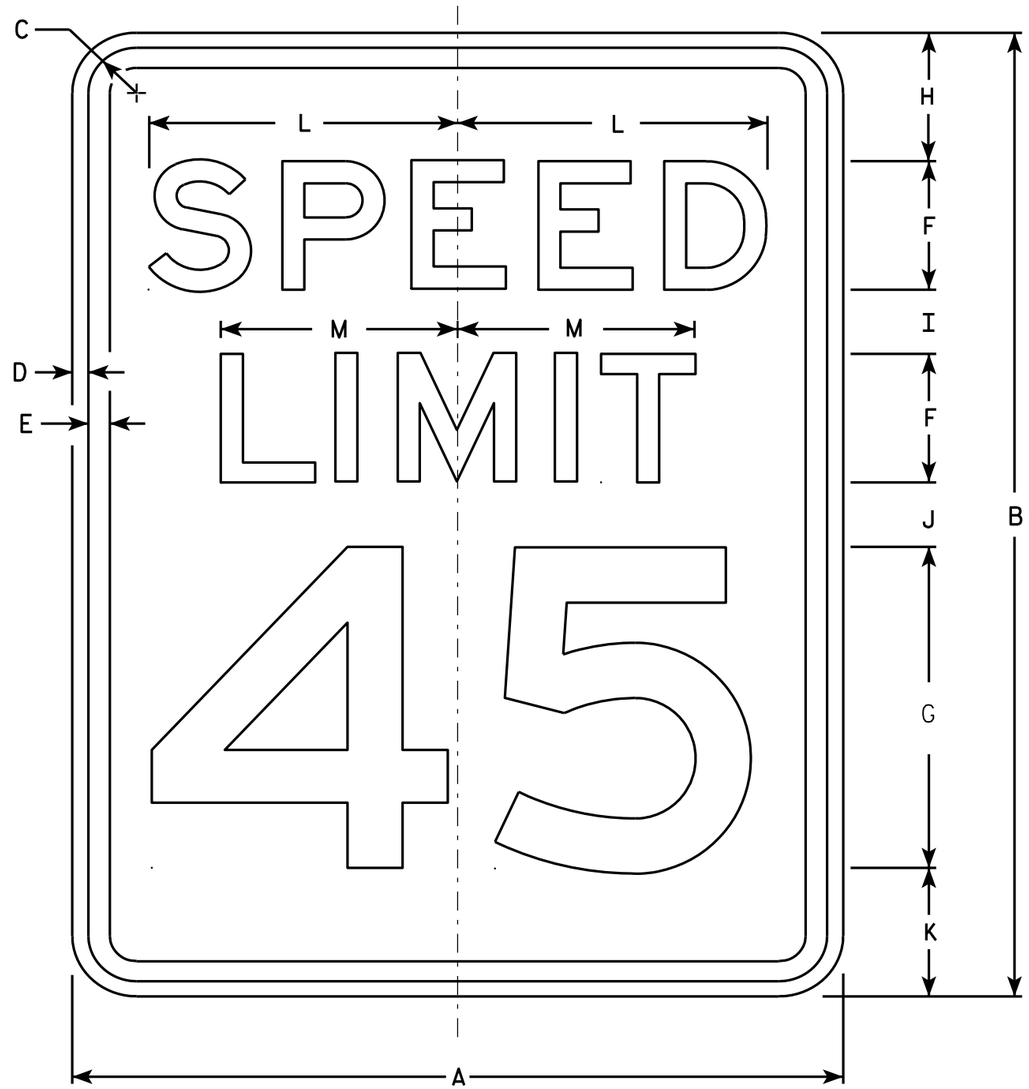
STANDARD SIGN
R1-54

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 12/03/10 PLATE NO. R1-54.2

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 152	E



R2-1

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White
   Message - Black
3. Message Series - E
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals and optically adjust spacing to achieve proper balance.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18	24	1 1/8	3/8	1/2	3	8	3	2	2	3	7 1/4	5 1/2														3.0
2S	24	30	1 1/8	3/8	1/2	4	10	3	2 1/4	3 3/8	3 3/8	9 5/8	7 3/8														5.0
2M	30	36	1 3/8	1/2	5/8	5	12	5	2 1/2	2 1/2	4	12	9 1/4														7.5
3	36	48	1 3/8	1/2	5/8	6	14	6	5	5	6	14 3/8	11														12.0
4	36	48	1 3/8	1/2	5/8	6	14	6	5	5	6	14 3/8	11														12.0
5	48	60	2 1/4	3/4	1	8	20	6	4 1/2	6 3/4	6 3/4	19 1/4	14 5/8														20.0

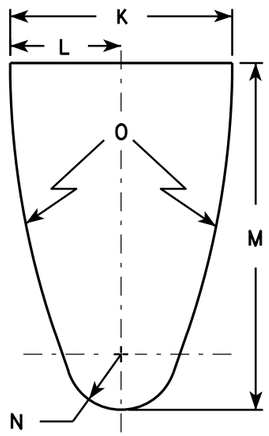
STANDARD SIGN
R2-1

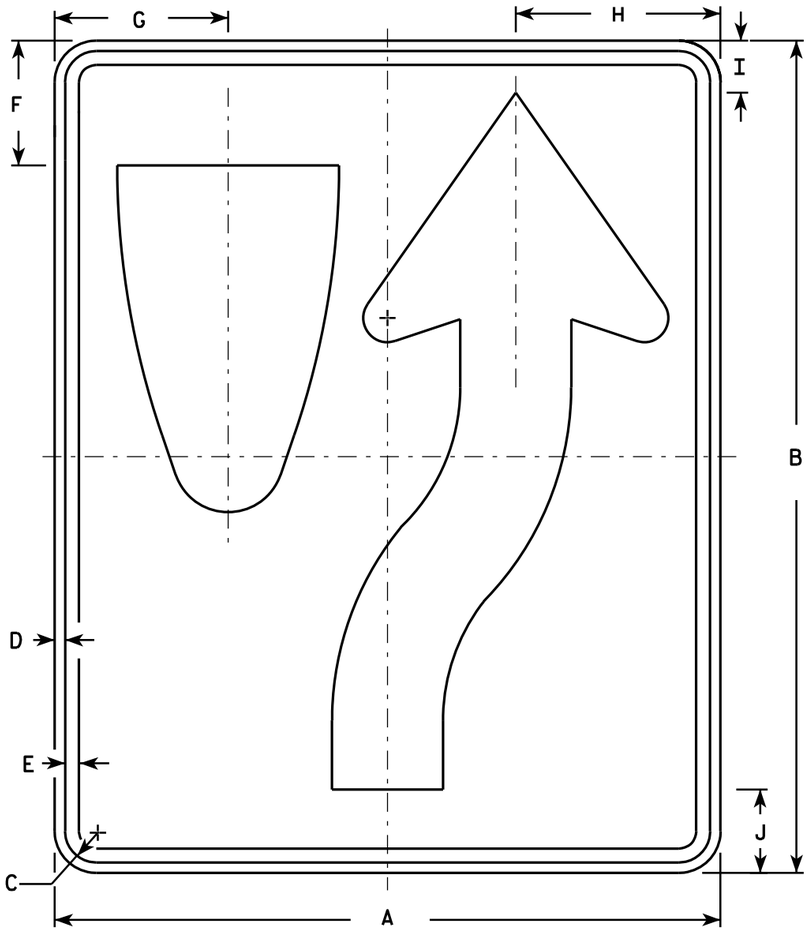
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 5/26/10 PLATE NO. R2-1.13

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 153 | E

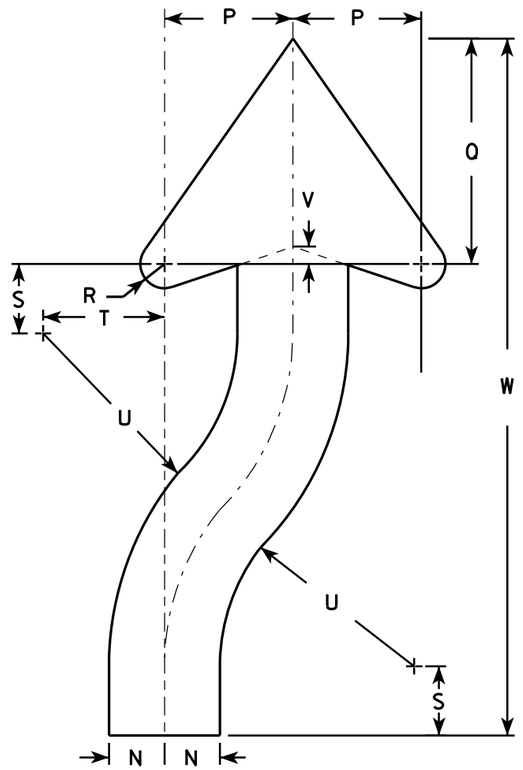




R4-7

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition. material is plywood but borders shall be rounded
2. Color:
   Background - White
   Message - Black
3. Corners may be square or rounded when base as shown. When base material is metal, the corners and borders shall be rounded.
4. R4-8 is the same as R4-7 except Legend is reversed.



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18	24	1 1/8	3/8	1/2	3 3/8	4 3/4	5 1/2	1 3/8	2 1/4	6	3	9 3/8	1 1/2	22 1/2	3 1/2	6 1/8	5/8	1 7/8	3 1/4	6 3/4	1/2	20 3/8				3.0
2S	24	30	1 1/8	3/8	1/2	4 1/2	6 1/4	7 3/8	1 7/8	3	8	4	12 1/2	2	30	4 5/8	8 1/8	7/8	2 1/2	4 3/8	9	5/8	25 1/8				5.0
2M	24	30	1 1/8	3/8	1/2	4 1/2	6 1/4	7 3/8	1 7/8	3	8	4	12 1/2	2	30	4 5/8	8 1/8	7/8	2 1/2	4 3/8	9	5/8	25 1/8				5.0
3	36	48	1 3/4	1/2	5/8	6 3/4	9 3/8	11 1/8	2 7/8	4 1/2	12	6	18 3/4	3	45	6 7/8	12 1/4	1 1/4	3 3/4	6 5/8	13 1/2	1	40 3/4				12.0
4	36	48	1 3/4	1/2	5/8	6 3/4	9 3/8	11 1/8	2 7/8	4 1/2	12	6	18 3/4	3	45	6 7/8	12 1/4	1 1/4	3 3/4	6 5/8	13 1/2	1	40 3/4				12.0
5	48	60	2 1/4	3/4	1	9	12 1/2	14 3/4	3 3/4	6	16	8	25	4	60	9 1/4	16 1/4	1 5/8	5	8 3/4	18	1 1/4	50 1/4				20.0

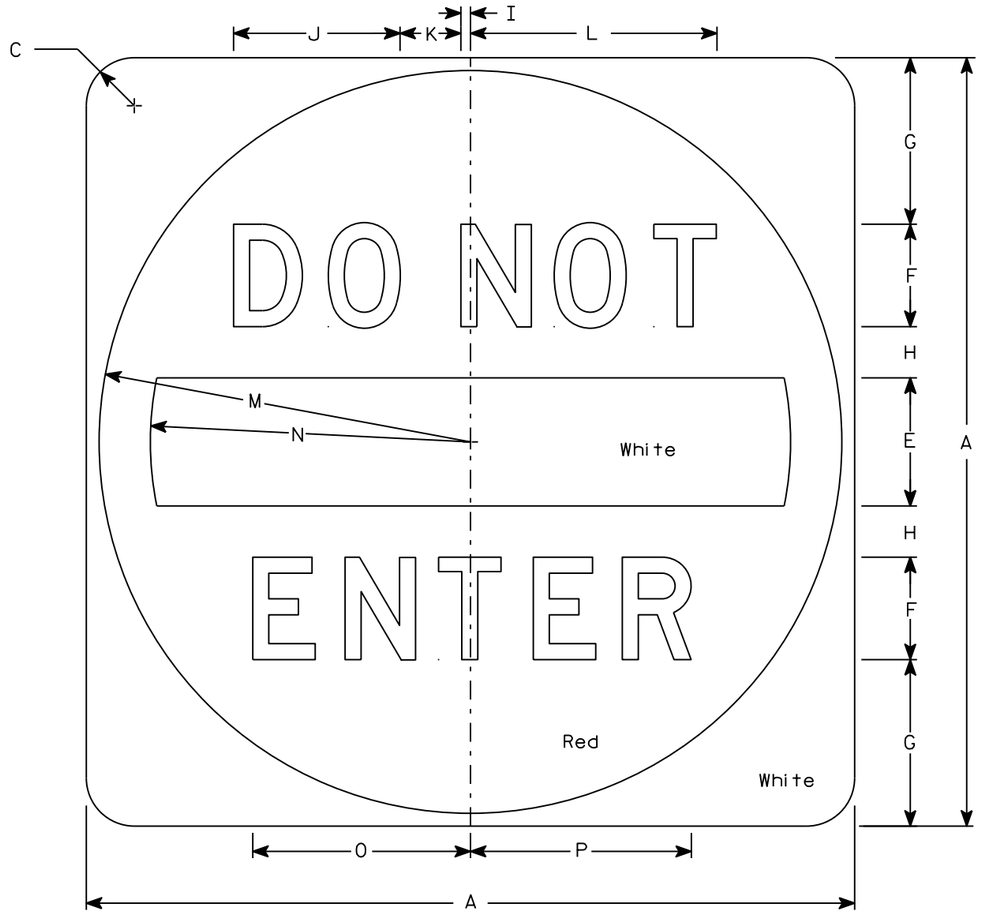
STANDARD SIGN
R4-7 & R4-8

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch for State Traffic Engineer

DATE 3/25/2011 PLATE NO. R4-7.8

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 154	E



R5-1

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
   Background - See detail
   Message - White
3. Message Series - D

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30		1 7/8		5	4	6 1/2	2	3/8	6 1/2	2 3/8	9 5/8	14 1/2	12 1/2	8 1/2	8 5/8											6.25
2M	36		2 1/4		6	5	7 1/2	2 1/2	1/2	8 1/8	3	12 1/8	17 1/2	15	10 5/8	10 3/4											9.0
3	36		2 1/4		6	5	7 1/2	2 1/2	1/2	8 1/8	3	12 1/8	17 1/2	15	10 5/8	10 3/4											9.0
4	36		2 1/4		6	5	7 1/2	2 1/2	1/2	8 1/8	3	12 1/8	17 1/2	15	10 5/8	10 3/4											9.0
5	48		3		8	6	11	3	5/8	9 3/4	3 5/8	14 1/2	23 1/2	20	12 3/4	12 7/8											16.0

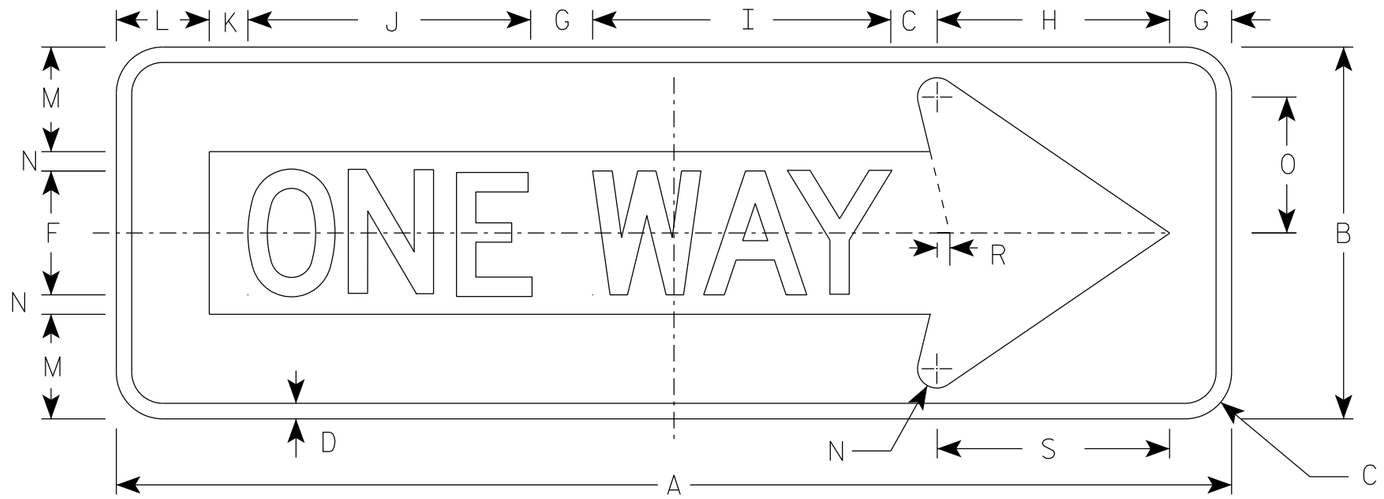
STANDARD SIGN
R5-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/15/18 PLATE NO. R5-1.16

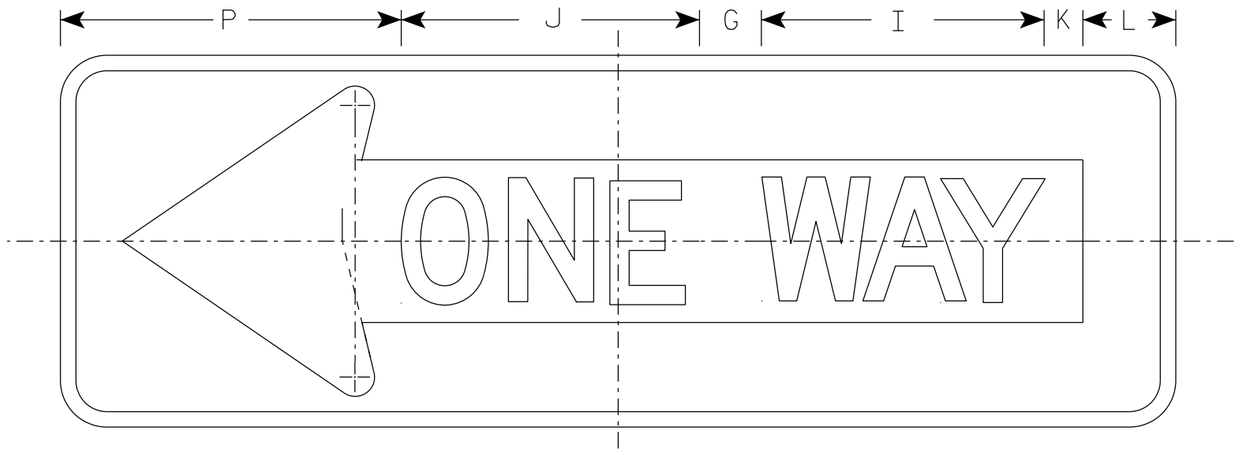
PROJECT NO: | HWY: | COUNTY: | SHEET NO: 155 | E



R6-1R

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
   Background - BLACK
   Message - BLACK LEGEND & WHITE ARROW & BORDER
3. Message Series - D



R6-1L

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	36	12	1 1/2	1/2		4	2	7 1/2	9 5/8	9 1/8	1 1/4	3	3 3/8	5/8	4 3/8	11		3/8	7 1/2								3.0
2M	54	18	2 1/4	3/4		6	3	11 1/4	13 5/8	14 1/2	1 7/8	4 1/2	5	1	6 1/2	16 1/2		5/8	11 1/4								6.75
3	54	18	2 1/4	3/4		6	3	11 1/4	13 5/8	14 1/2	1 7/8	4 1/2	5	1	6 1/2	16 1/2		5/8	11 1/4								6.75
4	54	18	2 1/4	3/4		6	3	11 1/4	13 5/8	14 1/2	1 7/8	4 1/2	5	1	6 1/2	16 1/2		5/8	11 1/4								6.75
5																											

STANDARD SIGN
R6-1 L & R

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

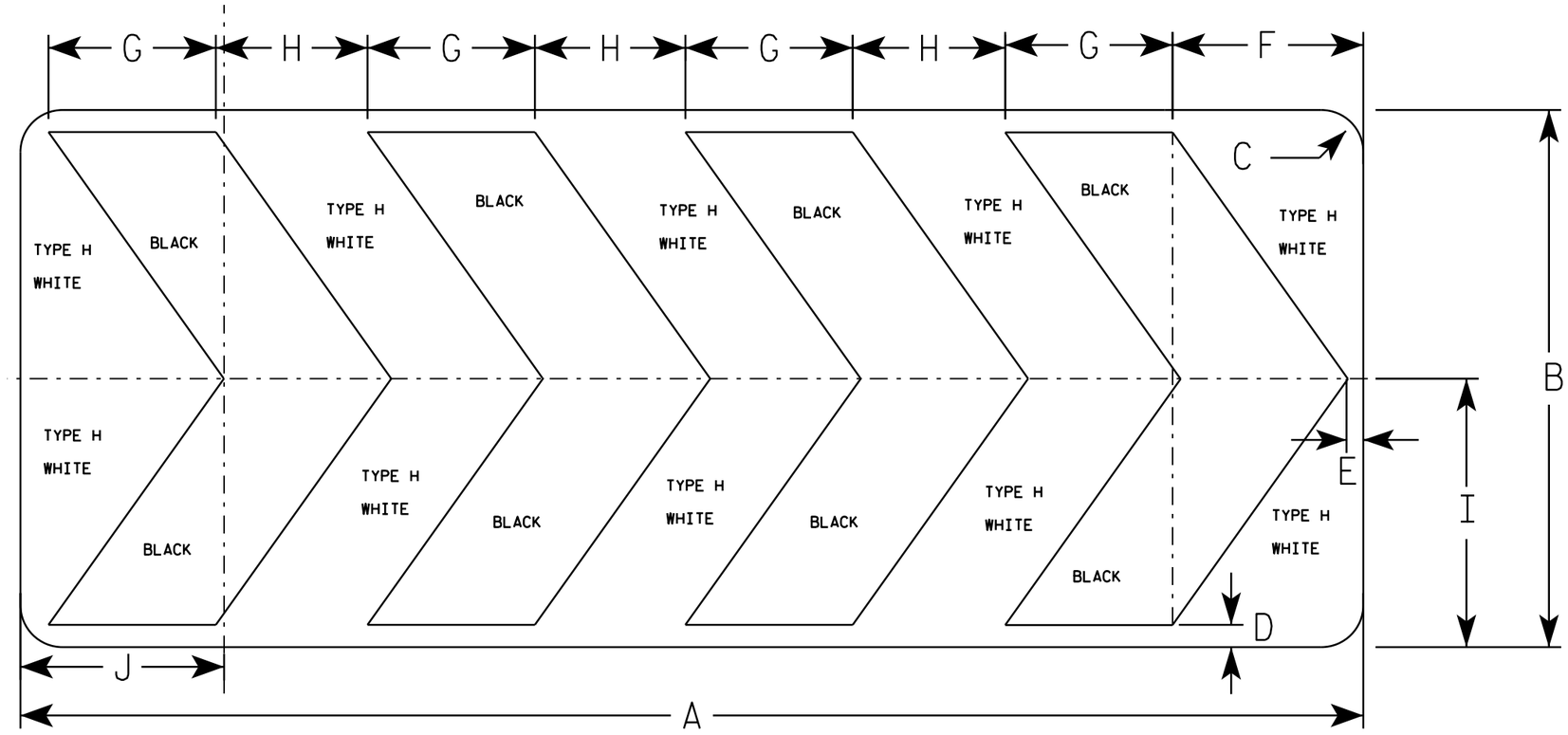
DATE 07/11/18 PLATE NO. R6-1.3

PROJECT NO:

SHEET NO: 156 E

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
   Background - WHITE
   Message - BLACK
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



R6-4B

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	60	24	1 7/8	1	3/4	8 1/2	7 1/2	6 3/4	12	9 1/8																	10.0
2M	60	24	1 7/8	1	3/4	8 1/2	7 1/2	6 3/4	12	9 1/8																	10.0
3																											
4																											
5																											

STANDARD SIGN
R6-4B

WISCONSIN DEPT OF TRANSPORTATION

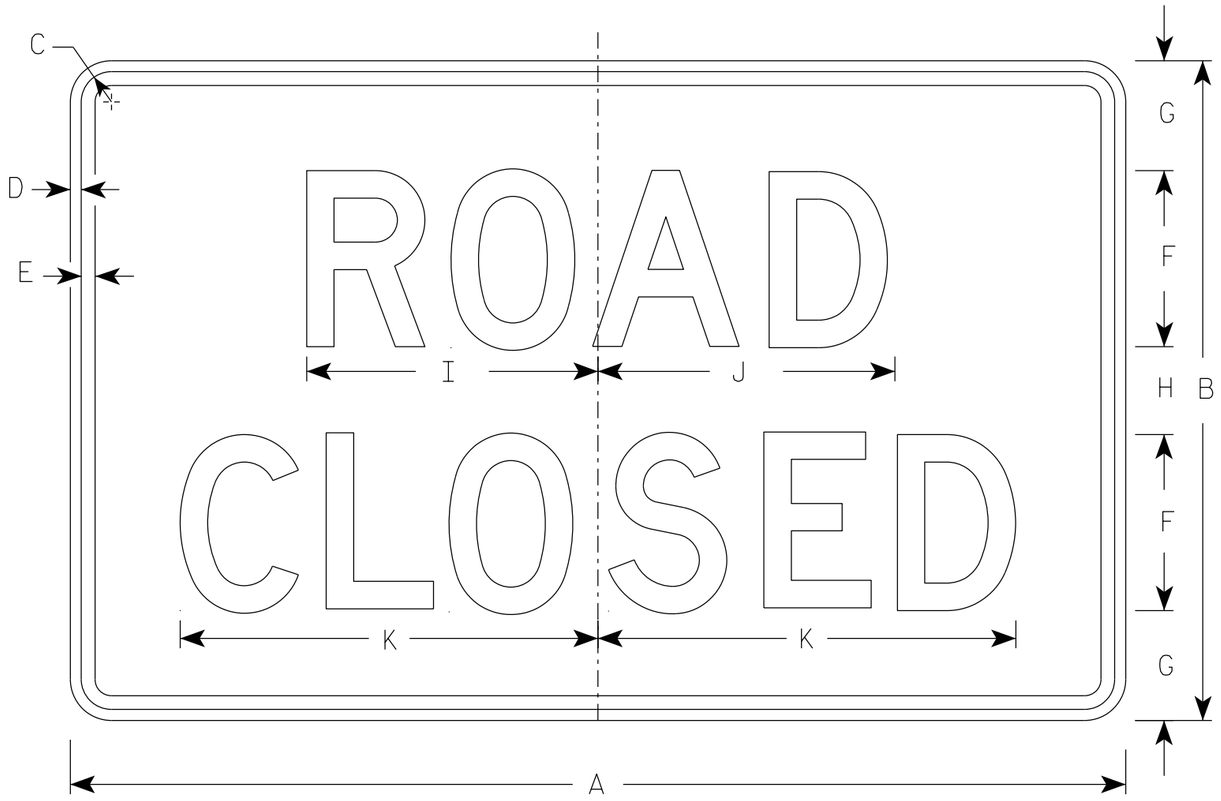
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 8/21/14 PLATE NO. R6-4.3

PROJECT NO: SHEET NO: E

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\R64B.DGN PLOT DATE : 21-AUG-2014 15:01 PLOT BY : mscsja

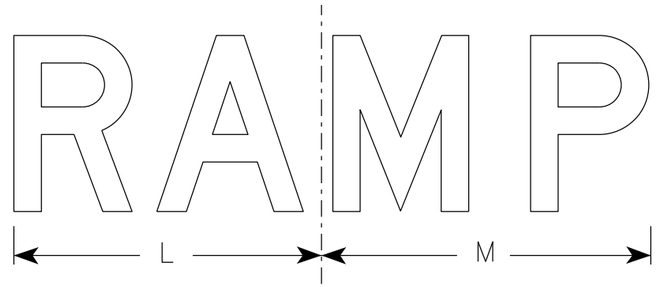
WISDOT/CADDS SHEET 42



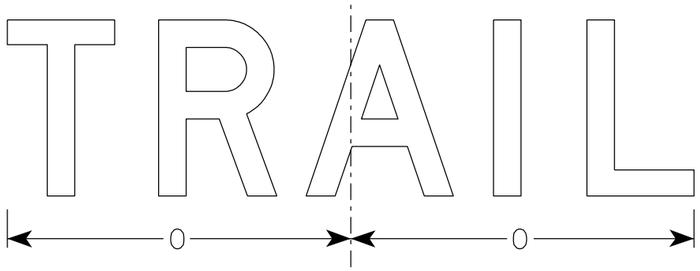
R11-2

NOTES

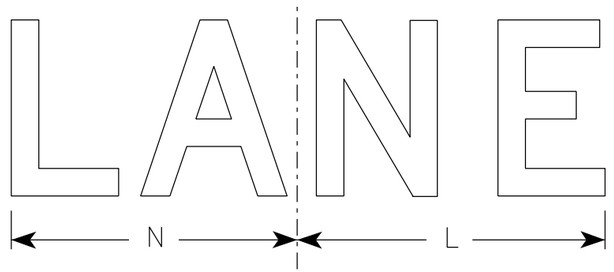
1. Sign is Type II - Type H Reflective
2. Color:
   Background - White
   Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Modify the message as required.



R11-2R



R11-2T



R11-2L

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
2M	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
3	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
4	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
5	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0

STANDARD SIGN
R11-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/29/2021 PLATE NO. R11-2.11

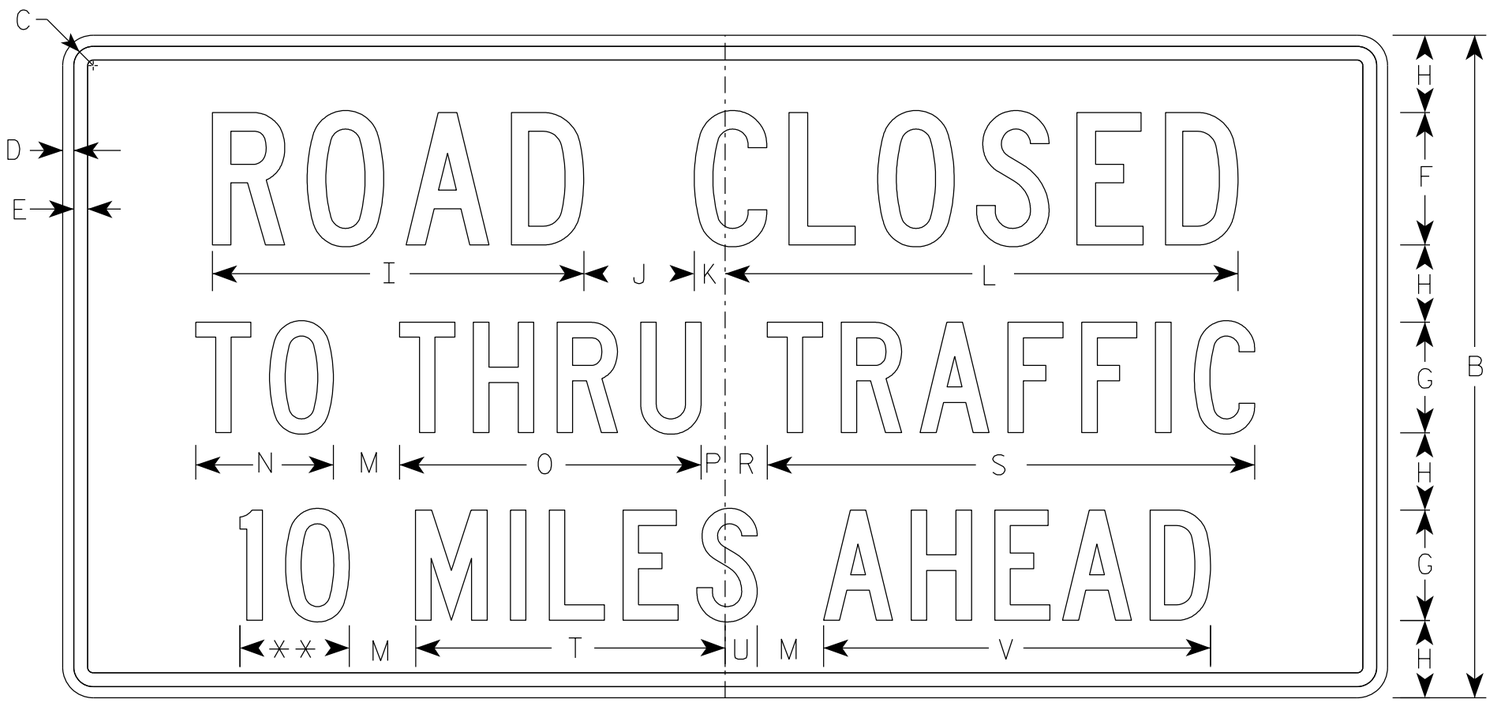
PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E

FILE NAME : C:\Users\PROJECTS\tr_stdplate\R112.dgn PLOT DATE : 29-MAR 2021 8:15 PLOT BY : dotc4c PLOT NAME : PLOT SCALE : $$.....plotscale.....$$ WISDOT/CADDS SHEET 42

7

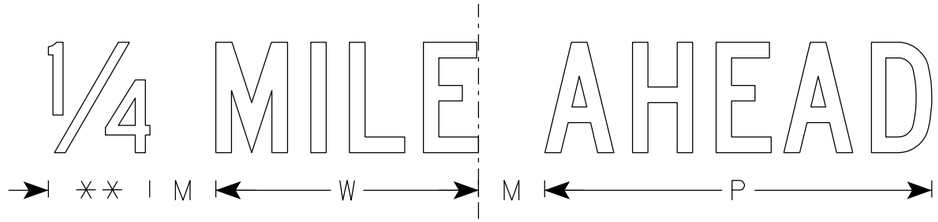
NOTES

1. Sign is Type II - Type H Reflective
2. Color:
   Background - White
   Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals to nearest quarter mile and optically adjust spacing to achieve proper balance.



R11-3

** See Note 5



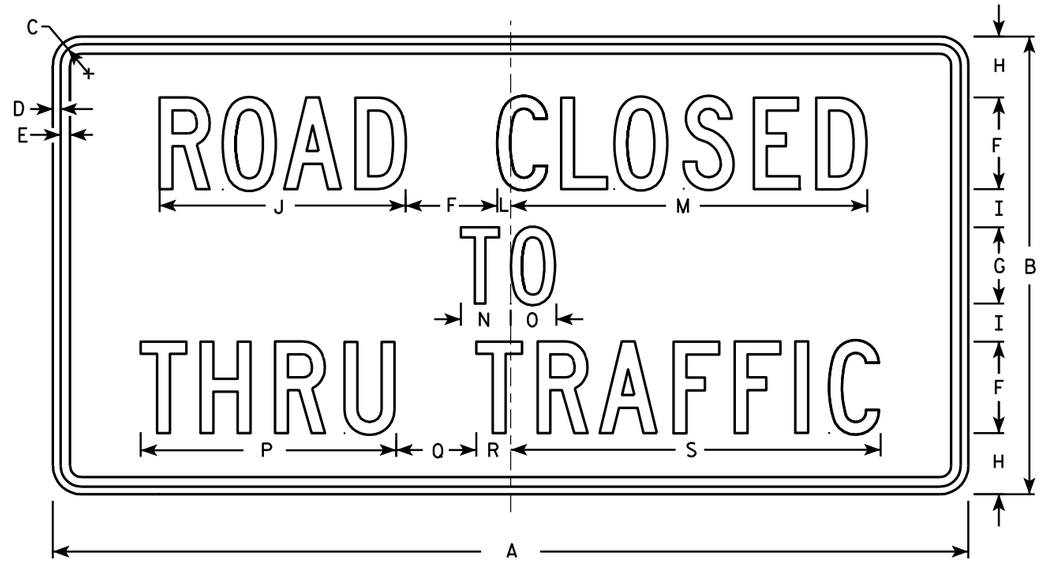
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36	18	1 1/4	3/8	3/8	4	3	2	11 1/4	3	1 1/8	15 3/8	2	3 3/4	8 1/4	5/8		1 3/8	13 1/4	8 3/8	7/8	10 1/2	7 1/8				4.5
2S	60	30	1 3/8	1/2	5/8	6	5	3 1/2	16 7/8	5	1 3/8	23 1/4	3	6 1/4	13 5/8	1 1/8		1 7/8	22 1/8	14	1 1/2	17 1/2	11 7/8				12.5
2M	60	30	1 3/8	1/2	5/8	6	5	3 1/2	16 7/8	5	1 3/8	23 1/4	3	6 1/4	13 5/8	1 1/8		1 7/8	22 1/8	14	1 1/2	17 1/2	11 7/8				12.5
3																											
4																											
5																											

STANDARD SIGN
R11-3

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer
DATE 6/14/2021 PLATE NO. R11-3.9

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 159	E



R11-4

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White
   Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	60	30	1 3/8	1/2	5/8	6	5	4	2 1/2	16 1/8		7/8	23 3/8	3 1/4	3	16 3/4	5 1/4	2 1/4	24 1/4								12.5
2M	60	30	1 3/8	1/2	5/8	6	5	4	2 1/2	16 1/8		7/8	23 3/8	3 1/4	3	16 3/4	5 1/4	2 1/4	24 1/4								12.5
3																											
4																											
5																											

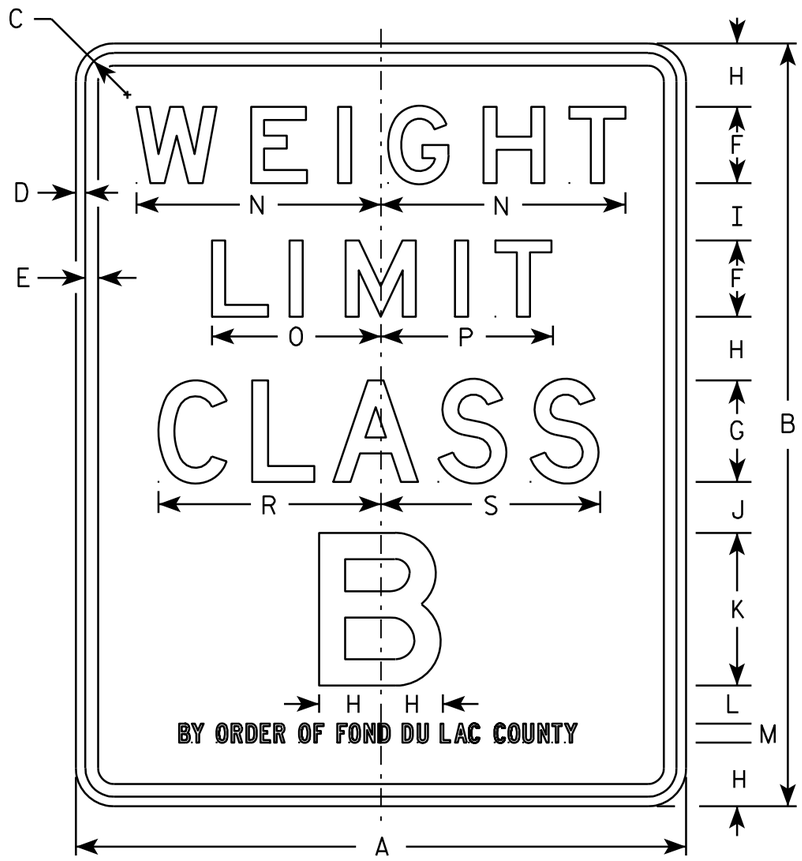
STANDARD SIGN
R11 - 4

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch for State Traffic Engineer

DATE 4/1/11 PLATE NO. R11-4.3

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 160	E



R12-53

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - White
   Message - Black
3. Message Series - See Note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Lines 1, 2 & 4 are Series E.
   Line 3 is Series D.
   Line 5 is Series C.
6. Line 5 of the sign shall vary with the name of the maintaining authority.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	30	1 1/8	3/8	1/2	3	4	2 1/2	2 1/4	2	6	1 1/2	3/4	9 5/8	6 5/8	6 3/4		8 3/4	8 5/8								5.0
2M	24	30	1 1/8	3/8	1/2	3	4	2 1/2	2 1/4	2	6	1 1/2	3/4	9 5/8	6 5/8	6 3/4		8 3/4	8 5/8								5.0
3																											
4																											
5																											

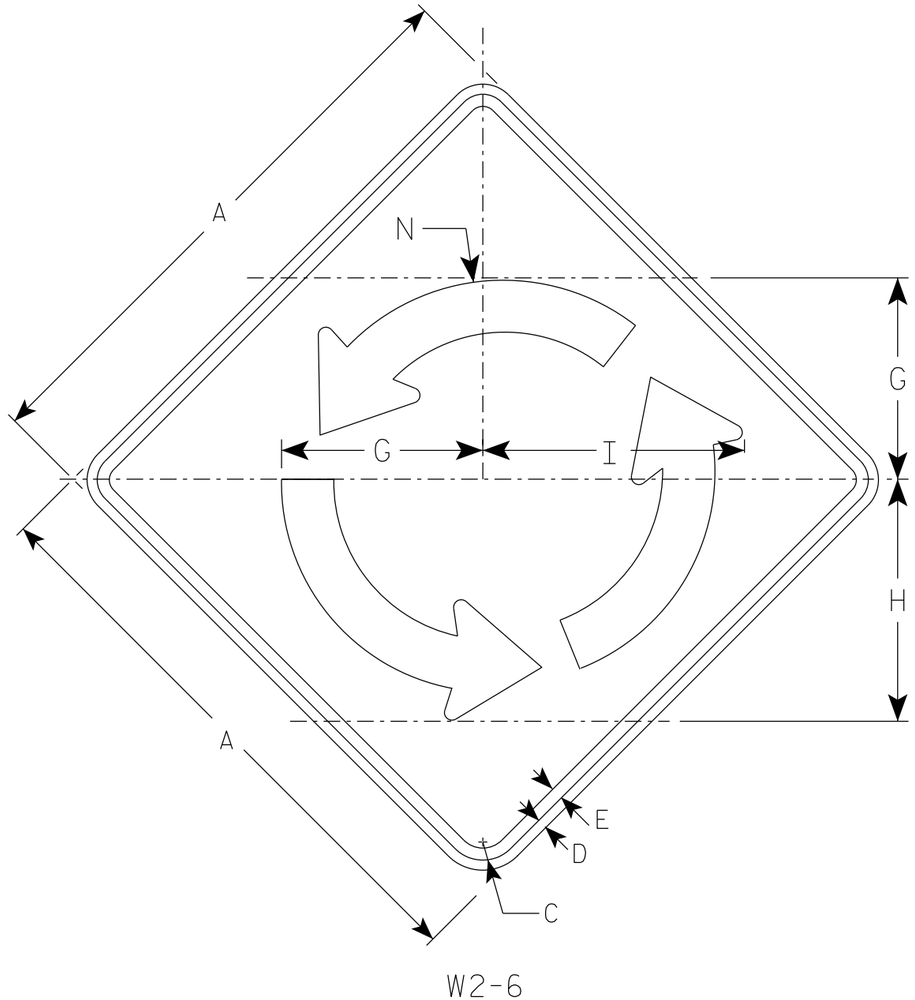
STANDARD SIGN
R12-53

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 4/1/11 PLATE NO. R12-53.3

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 161 | E

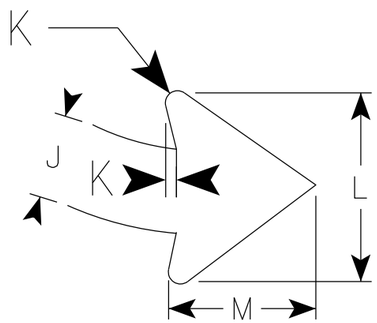


W2-6

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
   Background - Yellow
   Message - Black

Arrow Detail



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Area sq. ft.
1																									
2S	30		1 3/8	1/2	5/8		10 3/8	12 1/2	13 1/2	2 3/4	3/8	6	4 3/4	11 1/8											6.25
2M	30		1 3/8	1/2	5/8		10 3/8	12 1/2	13 1/2	2 3/4	3/8	6	4 3/4	11 1/8											6.25
3	36		1 5/8	5/8	3/4		12 1/2	15	16 1/4	3 1/4	1/2	7 3/8	5 3/4	13 3/8											9.00
4	48		2 1/4	3/4	1		16 5/8	20	16 1/4	4 3/8	5/8	9 3/4	7 5/8	17 7/8											16.0
5																									

STANDARD SIGN
W2-6

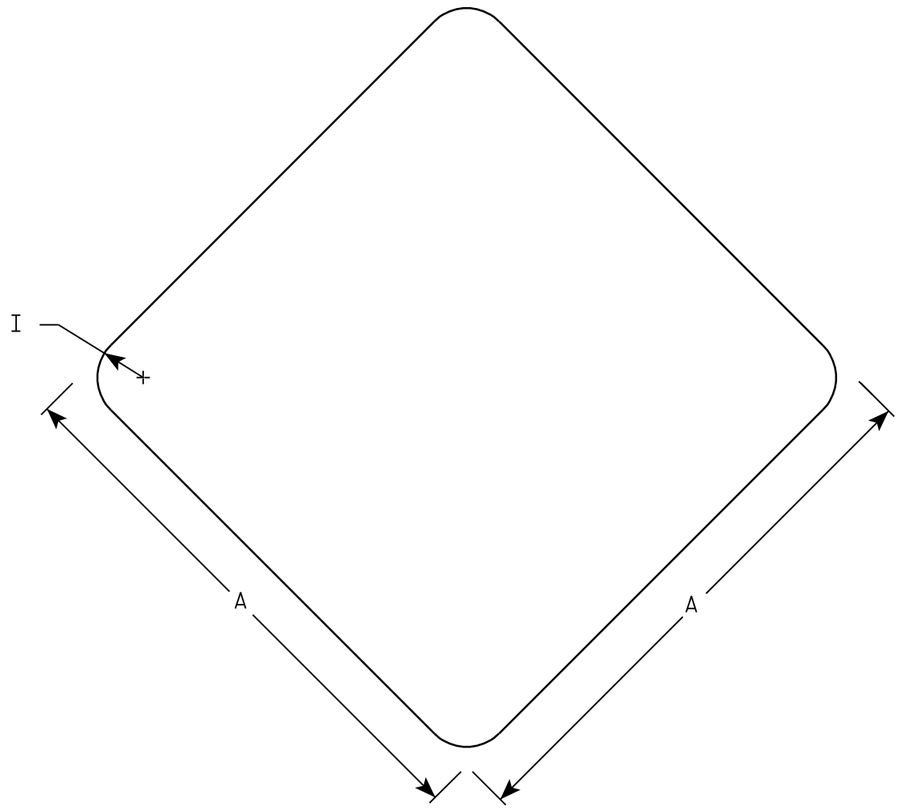
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/24/21 PLATE NO. W2-6.7

PROJECT NO:

SHEET NO: 162 E



W5-54

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Yellow
3. Corners may be square or rounded when base material is plywood. When base material is metal the corners shall be rounded.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	12								1																		1.0
2S	18								1 ½																		2.25
2M	18								1 ½																		2.25
3																											
4																											
5																											

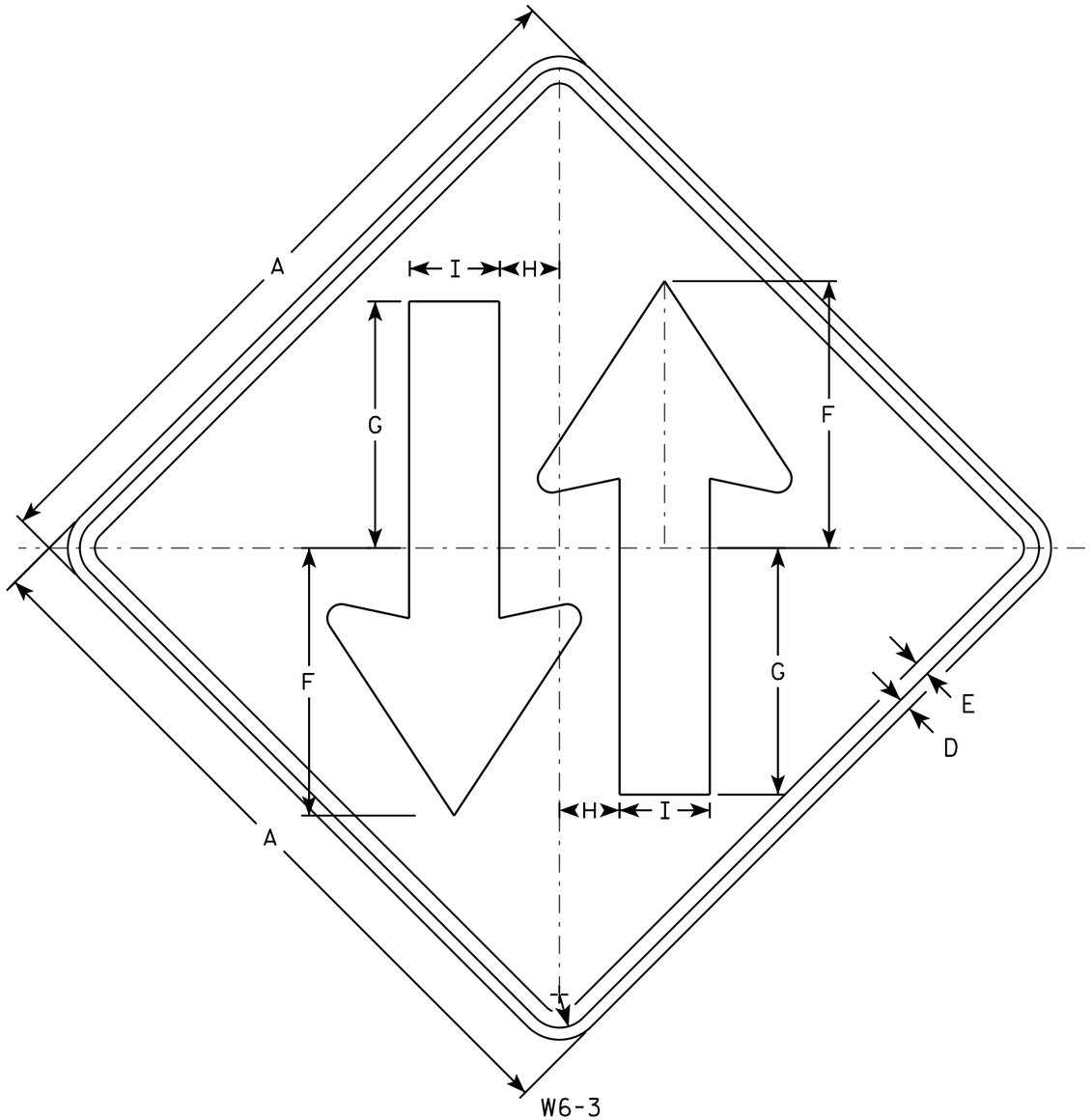
STANDARD SIGN
W5-54

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 11/3/10 PLATE NO. W5-54.8

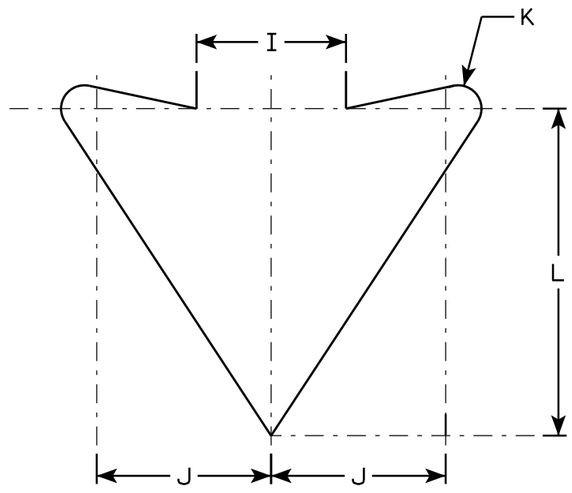
PROJECT NO: | HWY: | COUNTY: | SHEET NO: 163 | E



W6-3

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Yellow
   Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30		1 3/8	1/2	5/8	11 1/8	10 1/4	2 1/2	3 3/4	4 3/8	5/8	8 1/4															6.25
2S	36		1 5/8	5/8	3/4	13 3/8	12 1/4	3	4 1/2	5 1/4	3/4	9 7/8															9.0
2M	36		1 5/8	5/8	3/4	13 3/8	12 1/4	3	4 1/2	5 1/4	3/4	9 7/8															9.0
3																											
4	48		2 1/4	3/4	1	17 3/4	16 3/8	4	6	7	1	13 1/8															16.0
5	48		2 1/4	3/4	1	17 3/4	16 3/8	4	6	7	1	13 1/8															16.0

STANDARD SIGN
W6-3

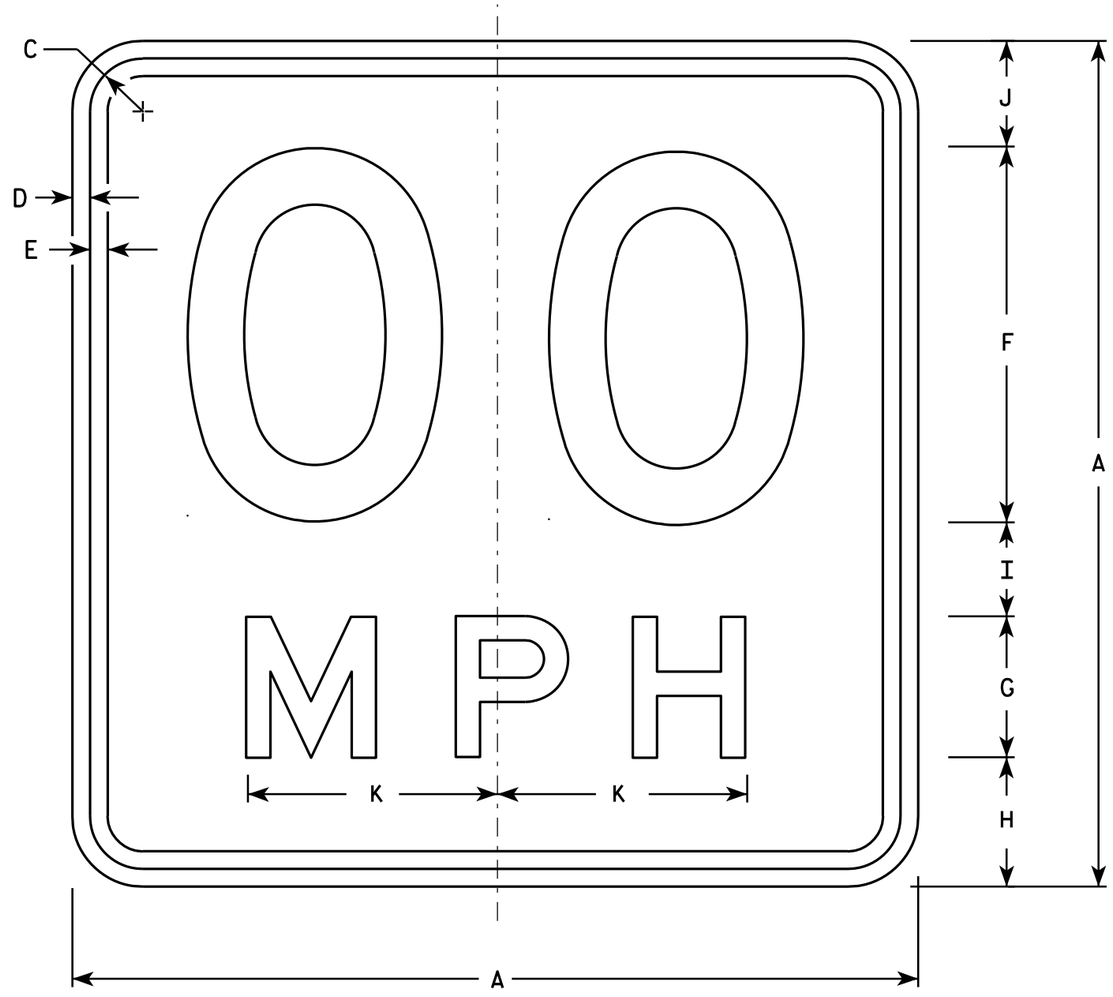
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/10/16 PLATE W6-3.11

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\W63.DGN PLOT DATE : 10-MAR-2016 14:01 PLOT BY : mscsja PLOT NAME : PLOT SCALE : 6.080757:1.000000 WISDOT/CADDS SHEET 42



W13-1

* For 30" x 30" Warning Signs, use 18" x 18" W13-1 signs.
For 36" x 36" Warning Signs, use 24" x 24" W13-1 signs.

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Yellow
   Message - Black
3. Message Series - See Note 6
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals and optically space about centerline to achieve proper balance.
6. Line 1 is Series D
   Line 2 is Series E

	SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
	1	18		1 1/8	3/8	3/8	8	3	2 3/4	2	2 1/4	5 3/8																2.25
*	2S	18		1 1/8	3/8	3/8	8	3	2 3/4	2	2 1/4	5 3/8																2.25
*	2M	18		1 1/8	3/8	3/8	8	3	2 3/4	2	2 1/4	5 3/8																2.25
	3	24		1 1/8	3/8	1/2	10	4	4	2 3/4	3 1/4	6 5/8																4.00
	4	36		1 5/8	5/8	3/4	16	6	5 1/2	4	4 1/2	10 5/8																9.00
	5	36		1 5/8	5/8	3/4	16	6	5 1/2	4	4 1/2	10 5/8																9.00

STANDARD SIGN
W13-1

WISCONSIN DEPT OF TRANSPORTATION

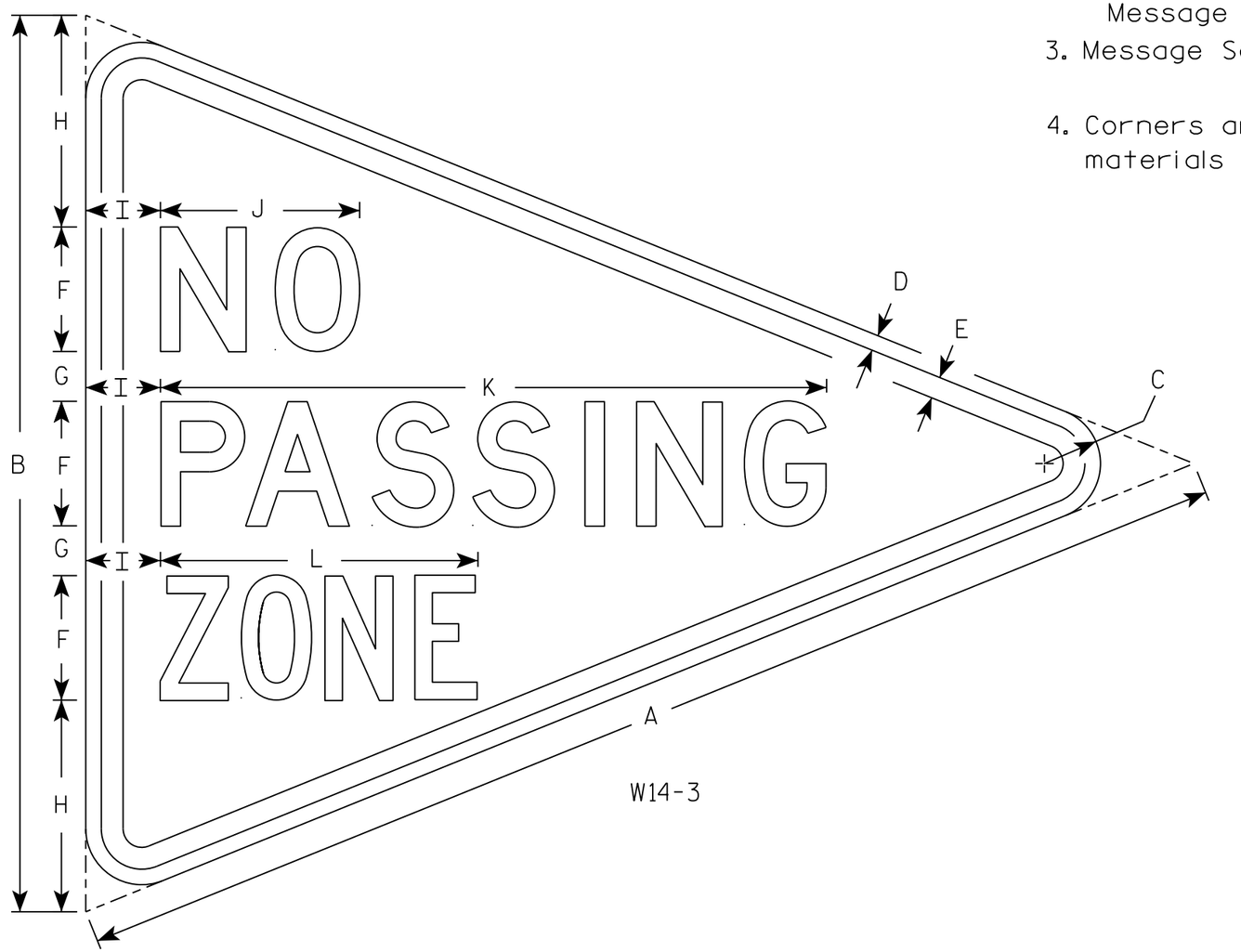
APPROVED Matthew R Rauch
For State Traffic Engineer

DATE 5/31/12 PLATE NO. W13-1.16

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 165 | E

NOTES

1. Sign is Type II- Type F Reflective
2. Color:
   Background - Yellow
   Message - Black
3. Message Series - Lines 1 and 2 are Series D.
   Line 3 is series C.
4. Corners and borders shall be rounded on all base materials for this sign.



W14-3

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48	36	2 1/4	5/8	7/8	5	2	8 1/2	3	8	26 3/4	12 3/4															5.56
2M																											
3																											
4																											
5																											

STANDARD SIGN
W14-3

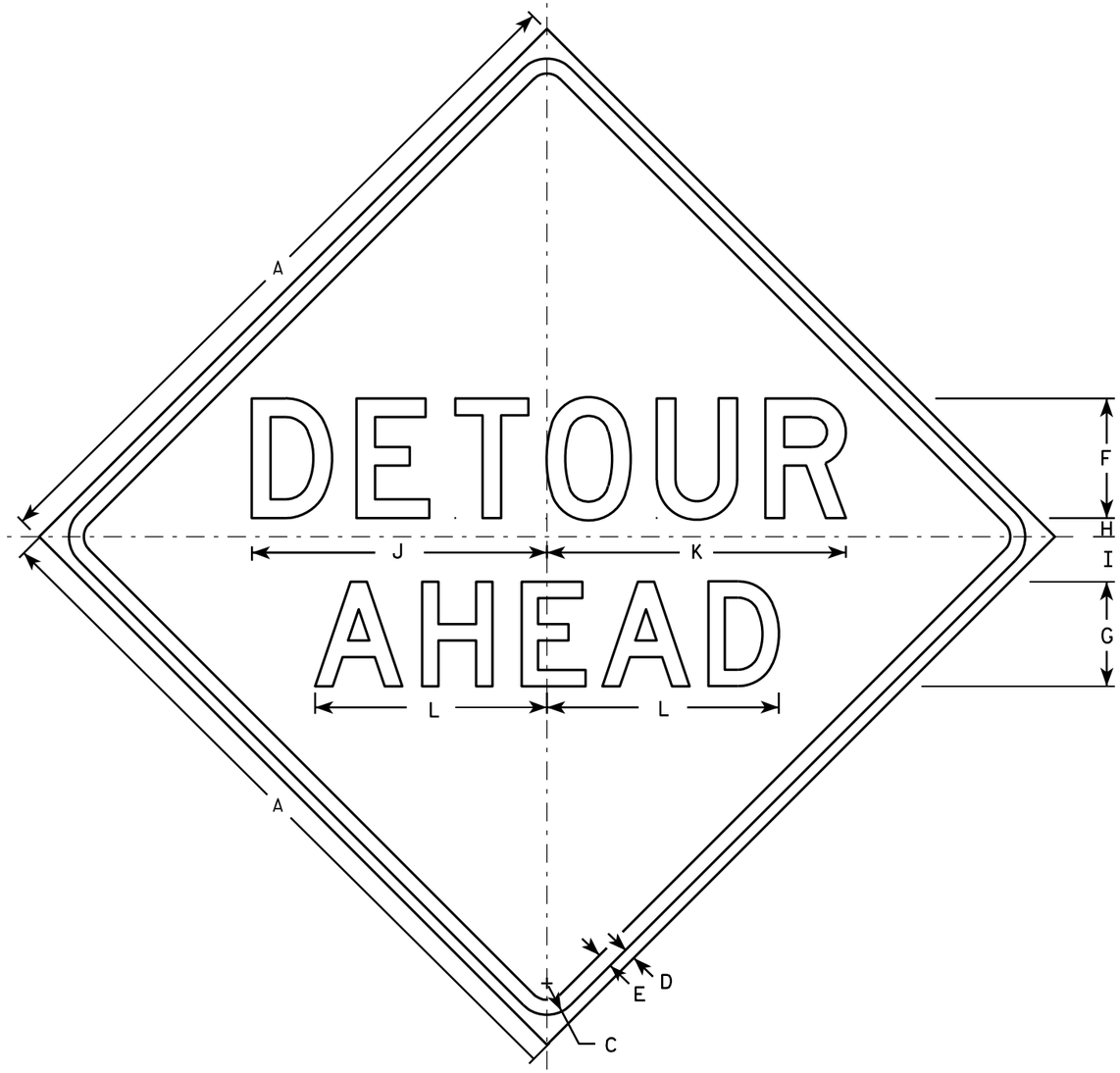
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

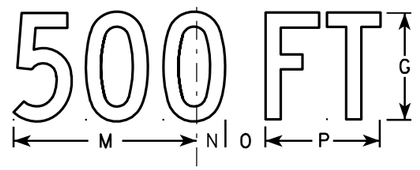
DATE 3/21/17 PLATE NO. W14-3.10

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 166 | E

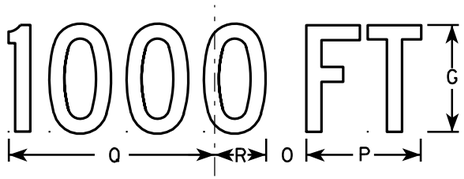
7



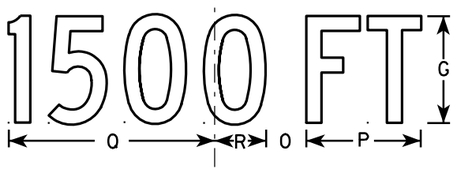
W20-2A



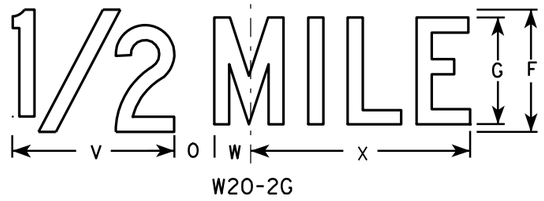
W20-2D



W20-2C



W20-2B



W20-2G

W20-2F

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Message Series - See note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Line 1 is Series D.
   Line 2 is Series D for AHEAD and Series C for all other distances.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	O	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	6	5	1	2 1/4	14 3/4	15	11 5/8	9	1 3/8	1 7/8	5 5/8	10 1/8	2 1/2	1 1/8	4 1/2	3 1/2	8	1 3/4	10 3/4			9.0
2S	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
2M	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
3	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
4	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
5	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0

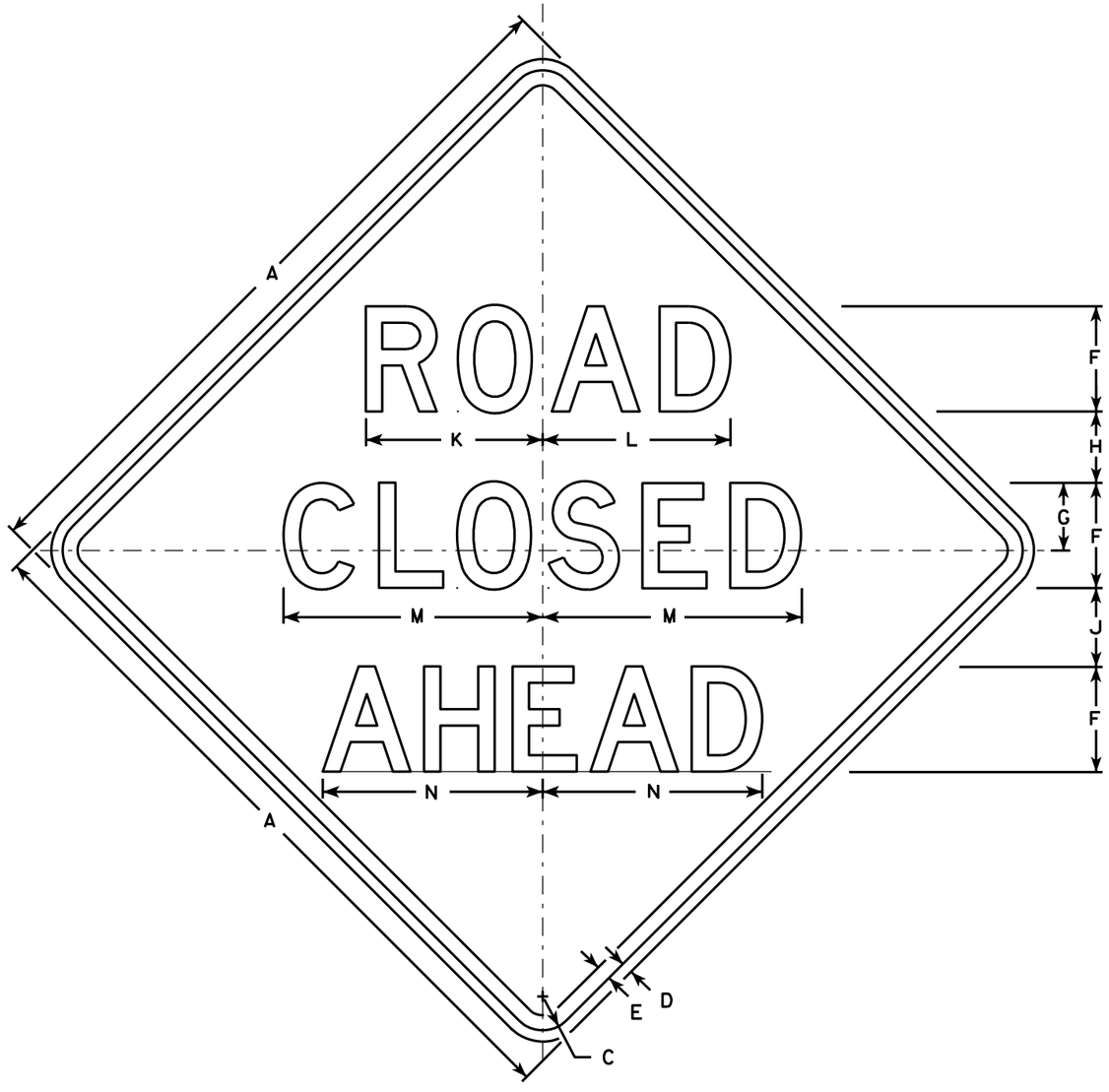
STANDARD SIGN
W20-2A,B,C,D,F & G

WISCONSIN DEPT OF TRANSPORTATION

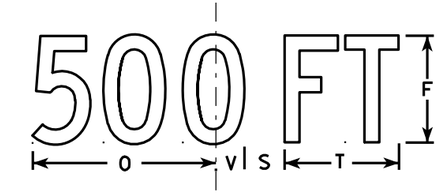
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/18/11 PLATE NO. W20-2.6

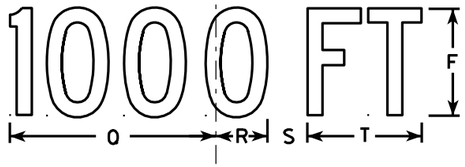
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 167	E



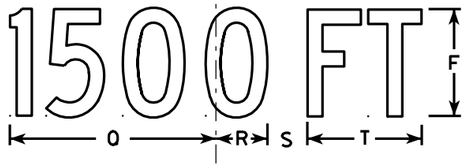
W20-3A



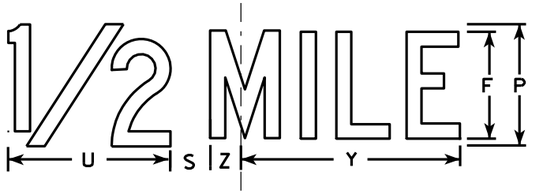
W20-3D



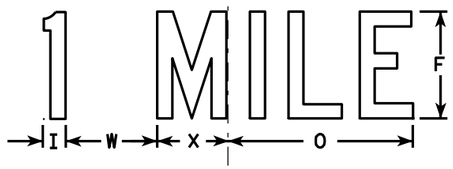
W20-3C



W20-3B



W20-3G



W20-3F

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
   Background - Orange
   Message - Black
3. Message Series - see note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Lines 1 and 2 are Series D.
   Line 3 is Series D for AHEAD and Series C for all other distances.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	5	3 3/8	3 1/2	1 1/8	4	8 3/8	8 7/8	12 1/2	11	9	6	10 1/8	2 1/2	1 7/8	5 5/8	8	1 3/8	4 1/2	3 1/2	10 3/4	1 3/4	9.0
2S	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
2M	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
3	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
4	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
5	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0

STANDARD SIGN
W20-3A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

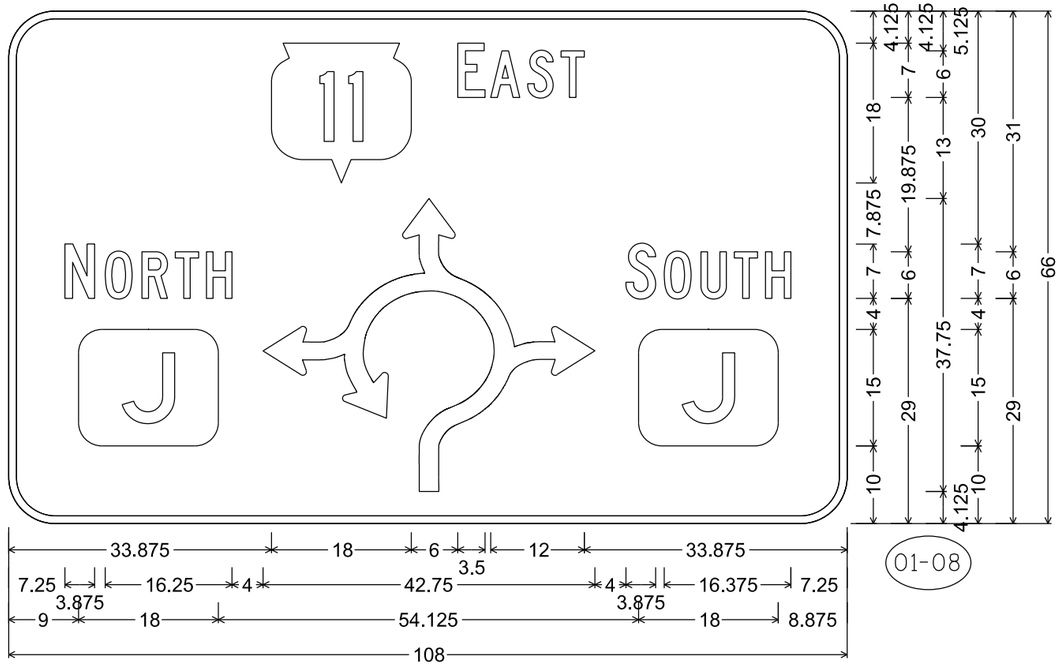
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/18/11 PLATE NO. W20-3.7

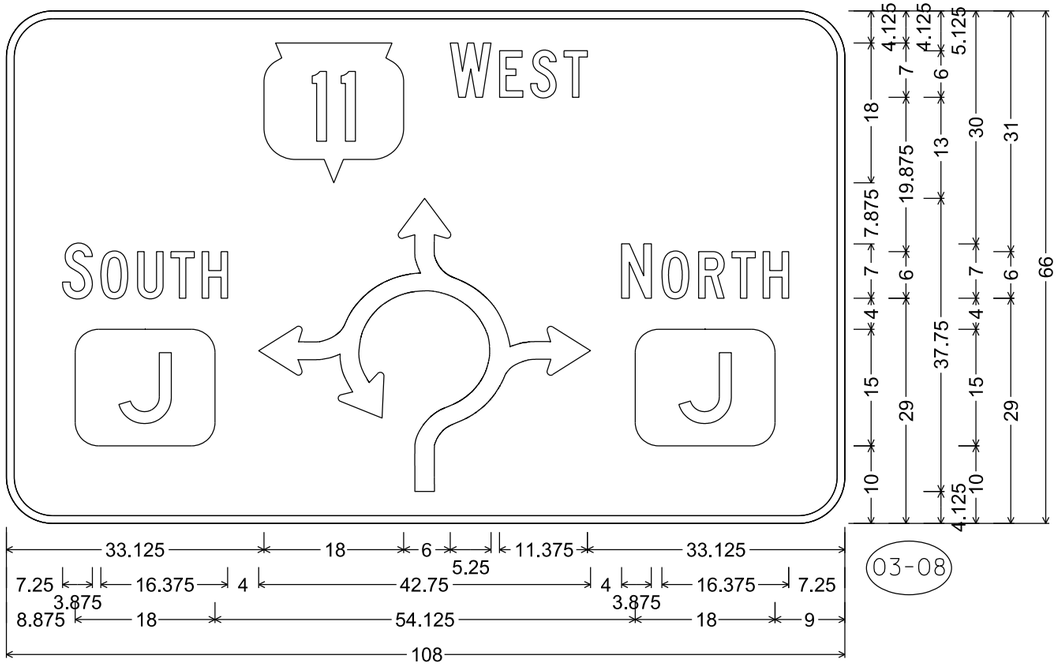
PROJECT NO: | HWY: | COUNTY: | SHEET NO: 168 | E

7

FILE NAME : C:\Users\PROJECTS\tr_stdplate\W203.DGN PLOT DATE : 18-MAR-2011 12:08 PLOT BY : mscj9h PLOT NAME : PLOT SCALE : 9.931739:1.000000
WISDOT/CADDS SHEET 42



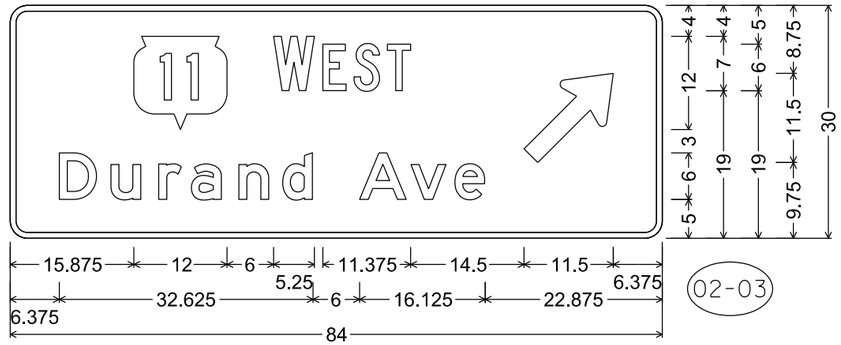
D1-62; 6.000" Radius, 1.000" Border



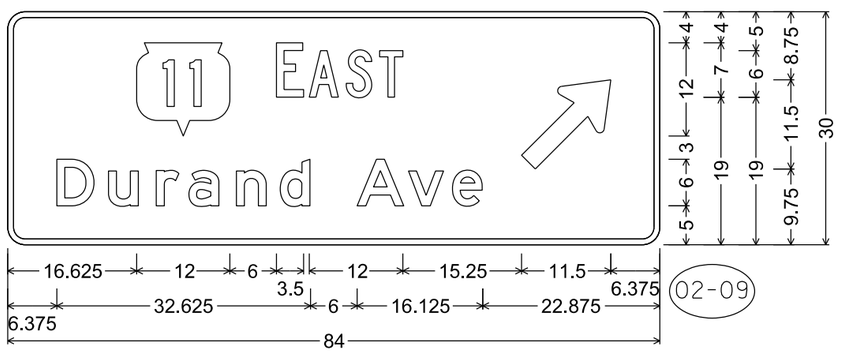
D1-62; 6.000" Radius, 1.000" Border

NOTES

1. All Signs Type II - Type H Reflective
2. Color:
   Background - Green
   Message - White
3. Message Series - C except as noted



D1-2; 2.250" Radius, 0.750" Border,
"WEST", C; "Durand", E; "Ave", E

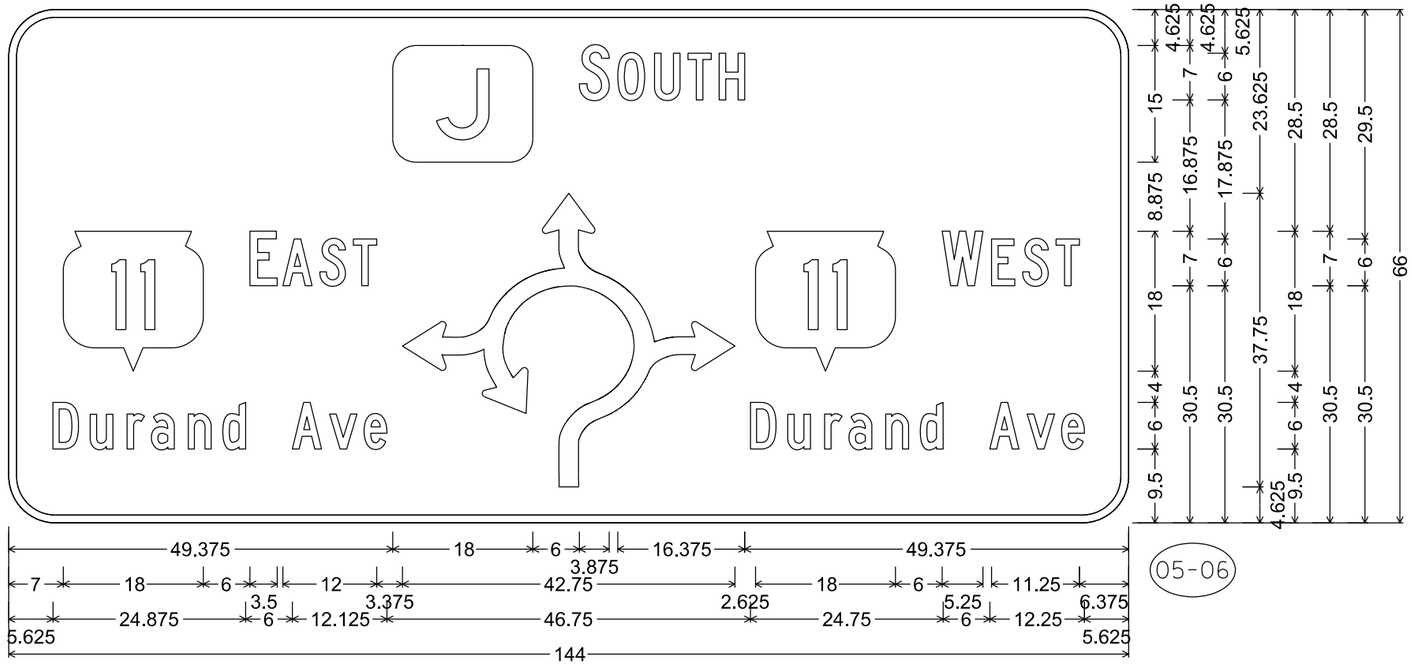


D1-2; 2.250" Radius, 0.750" Border,
"EAST", C; "Durand", E; "Ave", E

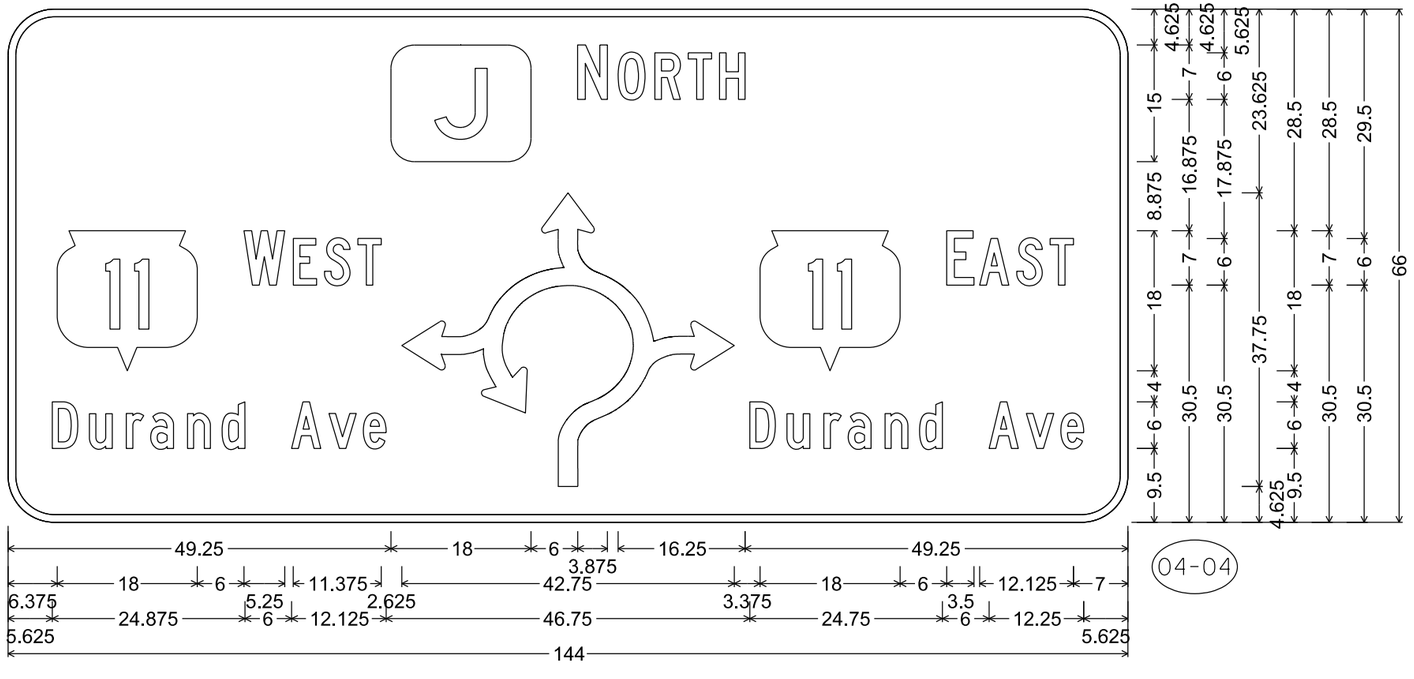
7

7

PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	PERMANENT SIGNING	SHEET NO: 169	E



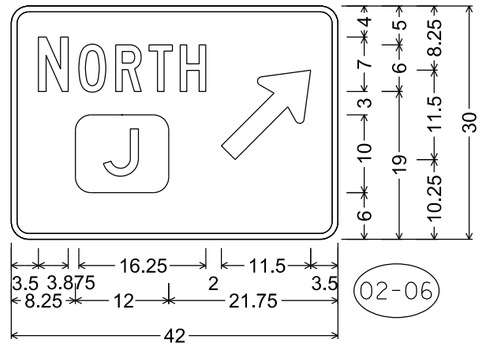
D1-62; 6.000" Radius, 1.000" Border



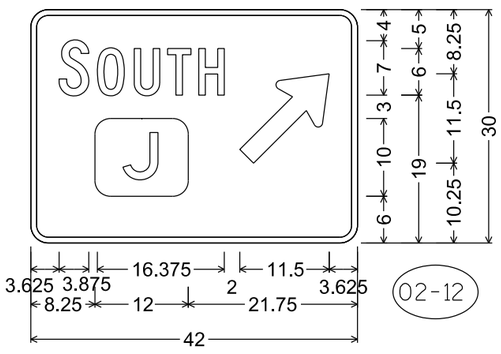
D1-62; 6.000" Radius, 1.000" Border

NOTES

1. All Signs Type II - Type H Reflective
2. Color:
   Background - Green
   Message - White
3. Message Series - C except as noted



D1-1; 2.250" Radius, 0.750" Border



D1-1; 2.250" Radius, 0.750" Border

EB - STH 11

CATEGORY	STATION	REAL STATION	DISTANCE	AREA (SF)			INCREMENTAL VOL (CY) (UNADJUSTED)			CUMULATIVE VOL (CY)		
				CUT	SALVAGED/UNUSABLE PAVEMENT MATERIAL	FILL	CUT	SALVAGED/UNUSABLE PAVEMENT MATERIAL	FILL	CUT	EXPANDED FILL	MASS ORDINATE
							NOTE 1	NOTE 2	NOTE 3	1.00 NOTE 1	1.25	NOTE 8
0010	205+11.40	20511.40	0.00	62.64	0.00	0.84	0	0	0	0	0	0
	205+50	20550.00	38.60	68.47	0.00	0.18	94	0	1	94	1	93
	206+00	20600.00	50.00	84.54	0.00	0.58	142	0	1	236	3	234
	206+50	20650.00	50.00	100.35	0.00	1.01	171	0	1	407	4	403
	207+00	20700.00	50.00	158.58	0.00	0.00	240	0	1	647	5	642
	207+50	20750.00	50.00	130.04	0.00	14.44	267	0	13	914	21	893
	208+00	20800.00	50.00	100.13	0.00	18.19	213	0	30	1,127	59	1,068
	208+50	20850.00	50.00	82.84	0.00	17.75	169	0	33	1,296	100	1,196
	209+00	20900.00	50.00	68.34	0.00	18.63	140	0	34	1,436	143	1,294
	209+50	20950.00	50.00	66.18	0.00	32.92	125	0	48	1,561	203	1,359
	210+00	21000.00	50.00	61.48	0.00	92.62	118	0	116	1,679	348	1,332
	210+50	21050.00	50.00	59.51	0.00	113.37	112	112	191	1,791	586	1,093
	212+50	21250.00	0.00	29.59	0.00	247.30	0	0	0	1,791	586	1,093
	213+00	21300.00	50.00	28.44	0.00	183.78	54	0	399	1,845	1,085	648
	213+50	21350.00	50.00	30.43	0.00	114.48	55	0	276	1,900	1,430	358
	214+00	21400.00	50.00	37.29	0.00	71.90	63	0	173	1,963	1,646	205
	214+50	21450.00	50.00	57.95	0.00	50.00	88	0	113	2,051	1,788	152
	215+00	21500.00	50.00	75.63	0.00	39.41	124	0	83	2,175	1,891	172
	215+50	21550.00	50.00	86.20	0.00	32.01	150	0	66	2,325	1,974	239
	216+00	21600.00	50.00	108.70	0.00	22.79	180	0	51	2,505	2,038	356
	216+50	21650.00	50.00	101.05	0.00	15.05	194	0	35	2,699	2,081	506
	217+00	21700.00	50.00	93.10	0.00	16.01	180	0	29	2,879	2,118	650
	217+50	21750.00	50.00	131.97	0.00	0.00	208	0	15	3,087	2,136	839
	217+76.03	21776.03	26.03	146.16	0.00	0.00	134	74	0	3,221	2,136	899
					PROJECT SUBTOTALS 0010		3,221	186	1,709			

NOTES:	
1 CUT	CUT INCLUDES SALVAGED/UNUSABLE PAVEMENT MATERIAL
2 - SALVAGED/UNUSABLE PAVEMENT MATERIAL	THIS DOES NOT SHOW UP IN CROSS SECTIONS
3 - FILL	DOES NOT INCLUDE UNUSABLE PAVEMENT EXC VOLUME
8 MASS ORDINATE	IF MARSH OR EBS TO BE BACKFILLED WITH COMMON OR BORROW: [(CUT SALVAGED PAVT EXPANDED MARSH EXC EXPANDED EBS) ((FILL REDUCED MARSH IN FILL REDUCED EBS IN FILL EXPANDED ROCK) * FILL FACTOR)]
8 - MASS ORDINATE	IF MARSH AND EBS TO BE BACKFILLED WITH GRANULAR: [CUT - SALVAGED PAVT - ((FILL - REDUCED MARSH IN FILL - REDUCED EBS IN FILL - EXPANDED ROCK) * FILL FACTOR)]
8 - MASS ORDINATE	IF MARSH AND EBS TO BE BACKFILLED WITH COMMON OR BORROW: [(CUT - SALVAGED PAVT - EXPANDED MARSH EXC - EXPANDED EBS) - ((FILL - EXPANDED ROCK) * FILL FACTOR)]
8 MASS ORDINATE	IF MARSH AND EBS TO BE BACKFILLED WITH GRANULAR: [CUT SALVAGED PAVT ((FILL EXPANDED ROCK) * FILL FACTOR)]

WB - STH 11

CATEGORY	STATION	REAL STATION	DISTANCE	AREA (SF)			INCREMENTAL VOL (CY) (UNADJUSTED)			CUMULATIVE VOL (CY)		
				CUT	SALVAGED/UNUSABLE PAVEMENT MATERIAL	FILL	CUT	SALVAGED/UNUSABLE PAVEMENT MATERIAL	FILL	CUT 1.00	EXPANDED FILL 1.25	MASS ORDINATE
							NOTE 1	NOTE 2	NOTE 3	NOTE 1		NOTE 8
0010	205+11.40	20511.40	0.00	64.14	0.00	4.51	0	0	0	0	0	0
	205+50	20550.00	38.60	64.74	0.00	4.90	92	0	7	92	9	83
	206+00	20600.00	50.00	64.91	0.00	4.99	120	0	9	212	20	192
	206+50	20650.00	50.00	66.03	0.00	3.98	121	0	8	333	30	303
	207+00	20700.00	50.00	68.74	0.00	2.28	125	0	6	458	38	421
	207+50	20750.00	50.00	70.11	0.00	1.31	129	0	3	587	41	546
	208+00	20800.00	50.00	89.56	0.00	0.42	148	0	2	735	44	691
	208+50	20850.00	50.00	97.56	0.00	0.10	173	0	0	908	44	864
	209+00	20900.00	50.00	121.19	0.00	0.00	203	0	0	1,111	44	1,067
	209+50	20950.00	50.00	123.66	0.00	0.02	227	0	0	1,338	44	1,294
	210+00	21000.00	50.00	83.42	0.00	4.00	192	0	4	1,530	49	1,481
	210+50	21050.00	50.00	68.22	0.00	18.47	140	225	21	1,670	75	1,370
	212+50	21246.01	0.00	144.30	0.00	40.15	0	0	0	1,670	75	1,370
	213+00	21296.01	50.00	137.02	0.00	22.27	260	0	58	1,930	148	1,558
	213+50	21346.01	50.00	150.63	0.00	8.93	266	0	29	2,196	184	1,787
	214+00	21396.01	50.00	133.41	0.00	3.41	263	0	11	2,459	198	2,037
	214+50	21446.01	50.00	108.50	0.00	2.58	224	0	6	2,683	205	2,253
	215+00	21496.01	50.00	95.65	0.00	1.67	189	0	4	2,872	210	2,437
	215+50	21546.01	50.00	91.38	0.00	0.72	173	0	2	3,045	213	2,608
	216+00	21596.01	50.00	71.74	0.00	0.00	151	0	1	3,196	214	2,757
	216+50	21646.01	50.00	67.50	0.00	0.00	129	0	0	3,325	214	2,886
	217+00	21696.01	50.00	64.99	0.00	0.82	123	0	1	3,448	215	3,008
	217+50	21746.01	50.00	62.99	0.00	4.55	119	0	5	3,567	221	3,121
	217+76.03	21776.03	30.02	64.43	0.00	3.92	71	249	5	3,638	228	2,937
				PROJECT SUBTOTALS 0010			3,638	474	182			

NOTES:	
1 - CUT	CUT INCLUDES SALVAGED/UNUSABLE PAVEMENT MATERIAL
2 - SALVAGED/UNUSABLE PAVEMENT MATERIAL	THIS DOES NOT SHOW UP IN CROSS SECTIONS
3 - FILL	DOES NOT INCLUDE UNUSABLE PAVEMENT EXC VOLUME
8 - MASS ORDINATE	IF MARSH OR EBS TO BE BACKFILLED WITH COMMON OR BORROW: [(CUT - SALVAGED PAVT - EXPANDED MARSH EXC - EXPANDED EBS) - ((FILL - REDUCED MARSH IN FILL - REDUCED EBS IN FILL - EXPANDED ROCK) * FILL FACTOR)]
8 - MASS ORDINATE	IF MARSH AND EBS TO BE BACKFILLED WITH GRANULAR: [CUT - SALVAGED PAVT - ((FILL - REDUCED MARSH IN FILL - REDUCED EBS IN FILL - EXPANDED ROCK) * FILL FACTOR)]
8 - MASS ORDINATE	IF MARSH AND EBS TO BE BACKFILLED WITH COMMON OR BORROW: [(CUT - SALVAGED PAVT - EXPANDED MARSH EXC - EXPANDED EBS) - ((FILL - EXPANDED ROCK) * FILL FACTOR)]
8 - MASS ORDINATE	IF MARSH AND EBS TO BE BACKFILLED WITH GRANULAR: [CUT - SALVAGED PAVT - ((FILL - EXPANDED ROCK) * FILL FACTOR)]

NB - CTH J

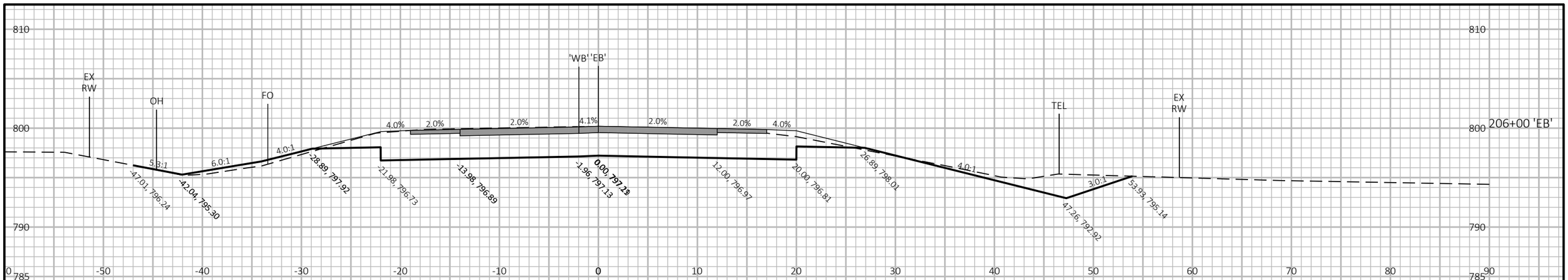
CATEGORY	STATION	REAL STATION	DISTANCE	AREA (SF)			INCREMENTAL VOL (CY) (UNADJUSTED)			CUMULATIVE VOL (CY)		
				CUT	SALVAGED/UNUSABLE PAVEMENT MATERIAL	FILL	CUT	SALVAGED/UNUSABLE PAVEMENT MATERIAL	FILL	CUT 1.00	FXPANDED FILL 1.25	MASS ORDINATE
							NOTE 1	NOTE 2	NOTE 3	NOTE 1		NOTE 8
0010	146+99.65	14699.65	0.00	71.70	0.00	0.00	0	0	0	0	0	0
	147+50	14750.00	50.35	75.57	0.00	0.00	137	0	0	137	0	137
	148+00	14800.00	50.00	90.18	0.00	0.29	153	0	0	290	0	290
	148+50	14850.00	50.00	107.64	0.00	3.41	183	0	3	473	4	469
	149+00	14900.00	50.00	120.96	0.00	10.85	212	0	13	685	20	665
	149+50	14950.00	50.00	130.88	0.00	0.02	233	0	10	918	33	886
	150+00	15000.00	50.00	99.69	0.00	16.36	213	0	15	1,131	51	1,080
	150+50	15050.00	50.00	88.77	0.00	63.65	174	0	74	1,305	144	1,161
	151+00	15100.00	50.00	83.76	0.00	110.97	160	62	162	1,465	346	1,057
	153+00	15300.00	0.00	46.05	0.00	124.04	0	0	0	1,465	346	1,057
	153+50	15350.00	50.00	49.09	0.00	84.23	88	0	193	1,553	588	904
	154+00	15400.00	50.00	80.91	0.00	28.34	120	0	104	1,673	718	894
	154+50	15450.00	50.00	78.88	0.00	18.86	148	0	44	1,821	773	987
	154+70	15470.00	20.00	69.23	0.00	19.03	55	0	14	1,876	790	1,024
	155+00	15500.00	30.00	85.33	0.00	13.84	86	0	18	1,962	813	1,088
	155+50	15550.00	50.00	74.51	0.00	6.18	148	0	19	2,110	836	1,212
	156+00	15600.00	50.00	71.16	0.00	2.34	135	0	8	2,245	846	1,337
	156+50	15650.00	50.00	67.55	0.00	1.23	128	0	3	2,373	850	1,461
	156+86.35	15686.35	36.35	65.85	0.00	0.28	90	80	1	2,463	851	1,470
				PROJECT SUBTOTALS 0010			2,463	142	681			

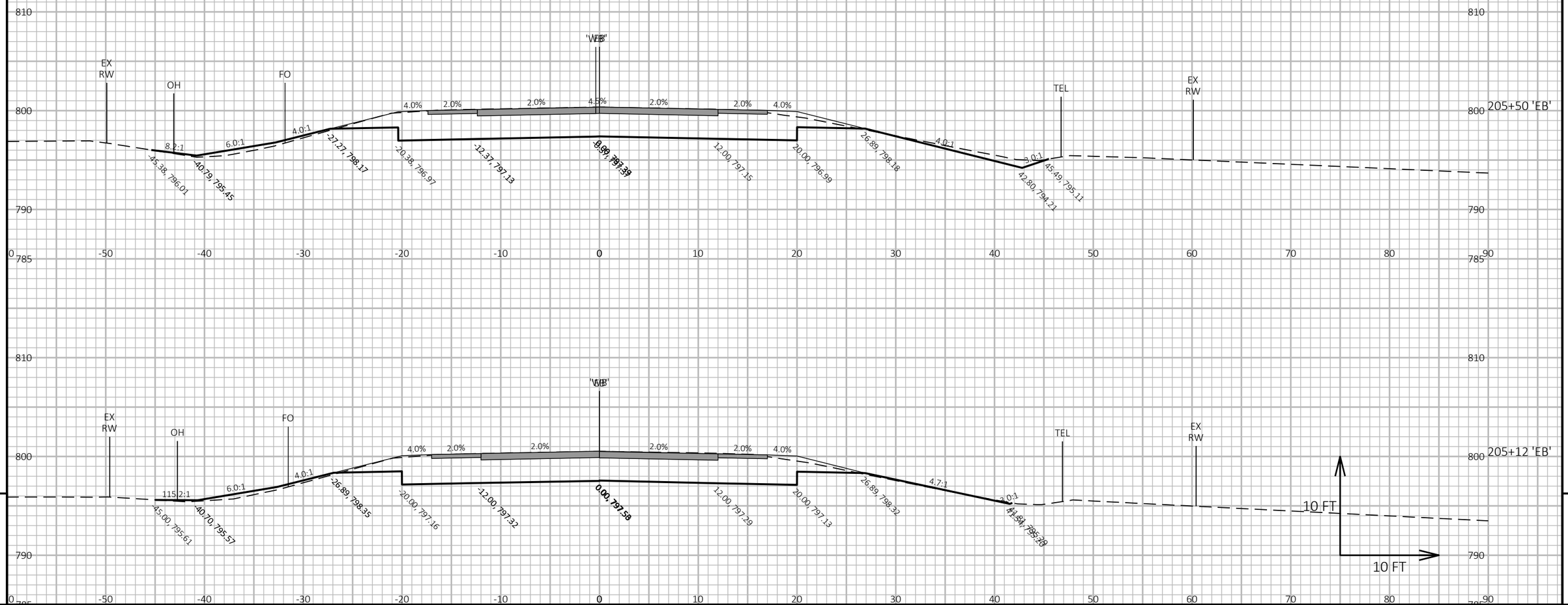
NOTES:	
1 - CUT	CUT INCLUDES SALVAGED/UNUSABLE PAVEMENT MATERIAL
2 - SALVAGED/UNUSABLE PAVEMENT MATERIAL	THIS DOES NOT SHOW UP IN CROSS SECTIONS
3 - FILL	DOES NOT INCLUDE UNUSABLE PAVEMENT EXC VOLUME
8 - MASS ORDINATE	IF MARSH OR EBS TO BE BACKFILLED WITH COMMON OR BORROW: [{CUT - SALVAGED PAVT - EXPANDED MARSH EXC - EXPANDED EBS} - {(FILL - REDUCED MARSH IN FILL - REDUCED EBS IN FILL - EXPANDED ROCK) * FILL FACTOR}]
8 - MASS ORDINATE	IF MARSH AND EBS TO BE BACKFILLED WITH GRANULAR: [CUT - SALVAGED PAVT - {(FILL - REDUCED MARSH IN FILL - REDUCED EBS IN FILL - EXPANDED ROCK} * FILL FACTOR)]
8 - MASS ORDINATE	IF MARSH AND EBS TO BE BACKFILLED WITH COMMON OR BORROW: [{CUT - SALVAGED PAVT - EXPANDED MARSH EXC - EXPANDED EBS} - {(FILL - EXPANDED ROCK} * FILL FACTOR}]
8 - MASS ORDINATE	IF MARSH AND EBS TO BE BACKFILLED WITH GRANULAR: [CUT - SALVAGED PAVT - {(FILL - EXPANDED ROCK} * FILL FACTOR)]

SB - CTH J

CATEGORY	STATION	REAL STATION	DISTANCE	AREA (SF)			INCREMENTAL VOL (CY) (UNADJUSTED)			CUMULATIVE VOL (CY)		
				CUT	SALVAGED/UNUSABLE PAVEMENT MATERIAL	FILL	CUT	SALVAGED/UNUSABLE PAVEMENT MATERIAL	FILL	CUT	EXPANDED FILL	MASS ORDINATE
										1.00	1.25	
							NOTE 1	NOTE 2	NOTE 3	NOTE 1		NOTE 8
0010	146+99.65	14699.65	0.00	62.30	0.00	0.74	0	0	0	0	0	0
	147+50	14750.00	50.35	69.65	0.00	0.00	123	0	1	123	1	122
	148+00	14800.00	50.00	79.39	0.00	0.00	138	0	0	261	1	260
	148+50	14850.00	50.00	86.75	0.00	0.00	154	0	0	415	1	414
	149+00	14900.00	50.00	95.63	0.00	0.65	169	0	1	584	3	582
	149+50	14950.00	50.00	117.09	0.00	0.98	197	0	2	781	5	776
	150+00	15000.00	50.00	114.74	0.00	2.32	215	0	3	996	9	987
	150+50	15050.00	50.00	104.99	0.00	45.61	203	0	44	1,199	64	1,135
	151+00	15100.00	50.00	91.57	0.00	85.45	182	77	121	1,381	215	1,089
	153+00	15300.10	0.00	65.06	0.00	26.65	0	0	0	1,381	215	1,089
	153+50	15350.10	50.00	60.50	0.00	3.47	116	0	28	1,497	250	1,170
	153+75	15375.10	25.00	58.79	0.00	0.01	55	0	2	1,552	253	1,223
	154+00	15400.10	25.00	83.68	0.00	0.84	66	0	0	1,618	253	1,289
	154+33	15433.00	32.90	61.56	0.00	0.00	88	0	1	1,706	254	1,375
	154+50	15450.10	17.10	60.58	0.00	1.51	39	0	0	1,745	254	1,414
	154+70	15470.10	20.00	72.22	0.00	1.69	49	0	1	1,794	255	1,462
	155+00	15500.10	30.00	55.15	0.00	2.94	71	0	3	1,865	259	1,529
	155+50	15550.10	50.00	55.50	0.00	2.97	102	0	5	1,967	265	1,625
	156+00	15600.10	50.00	57.36	0.00	2.27	104	0	5	2,071	271	1,723
	156+50	15650.10	50.00	59.64	0.00	1.73	108	0	4	2,179	276	1,826
	156+86.35	15686.35	36.25	60.30	0.00	2.13	81	124	2	2,260	279	1,790
					PROJECT SUBTOTATLS 0010		2,260	191	223			

NOTES:	
1 CUT	CUT INCLUDES SALVAGED/UNUSABLE PAVEMENT MATERIAL
2 - SALVAGED/UNUSABLE PAVEMENT MATERIAL	THIS DOES NOT SHOW UP IN CROSS SECTIONS
3 - FILL	DOES NOT INCLUDE UNUSABLE PAVEMENT EXC VOLUME
8 MASS ORDINATE	IF MARSH OR EBS TO BE BACKFILLED WITH COMMON OR BORROW: [{CUT SALVAGED PAVT EXPANDED MARSH EXC EXPANDED EBS} {{FILL REDUCED MARSH IN FILL REDUCED EBS IN FILL EXPANDED ROCK) * FILL FACTOR)]
8 - MASS ORDINATE	IF MARSH AND EBS TO BE BACKFILLED WITH GRANULAR: [CUT - SALVAGED PAVT - {(FILL - REDUCED MARSH IN FILL - REDUCED EBS IN FILL - EXPANDED ROCK) * FILL FACTOR}]
8 - MASS ORDINATE	IF MARSH AND EBS TO BE BACKFILLED WITH COMMON OR BORROW: [{CUT - SALVAGED PAVT - EXPANDED MARSH EXC - EXPANDED EBS} - {(FILL - EXPANDED ROCK) * FILL FACTOR}]
8 MASS ORDINATE	IF MARSH AND EBS TO BE BACKFILLED WITH GRANULAR: [CUT SALVAGED PAVT {(FILL EXPANDED ROCK) * FILL FACTOR)]





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9

PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	CROSS SECTIONS: 'EB'	SHEET 175	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\090202-XS.DWG
LAYOUT NAME - EB1

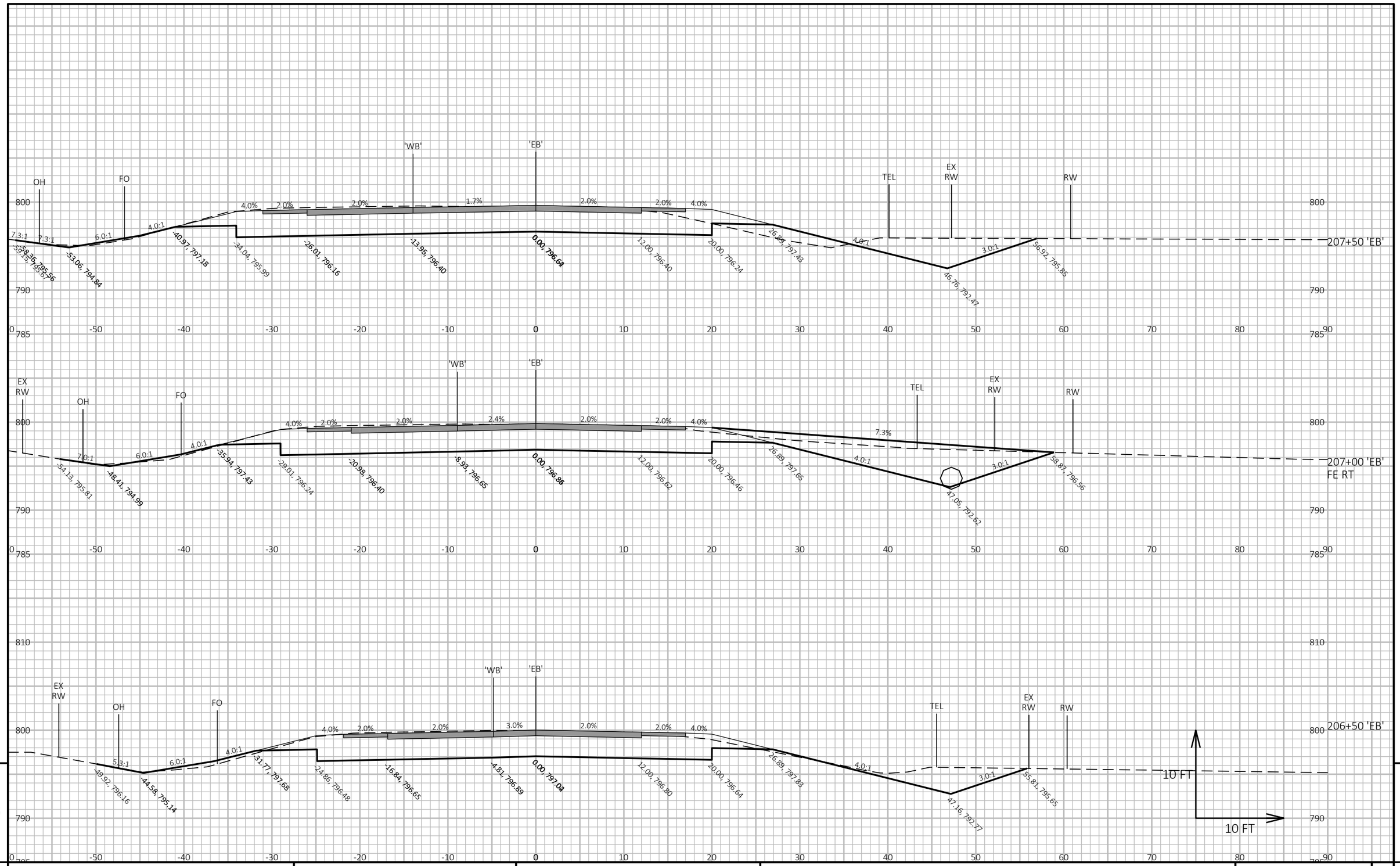
PLOT DATE : 10/27/2023 2:55 PM

PLOT BY : KUSCHEL, LEVI

PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



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9

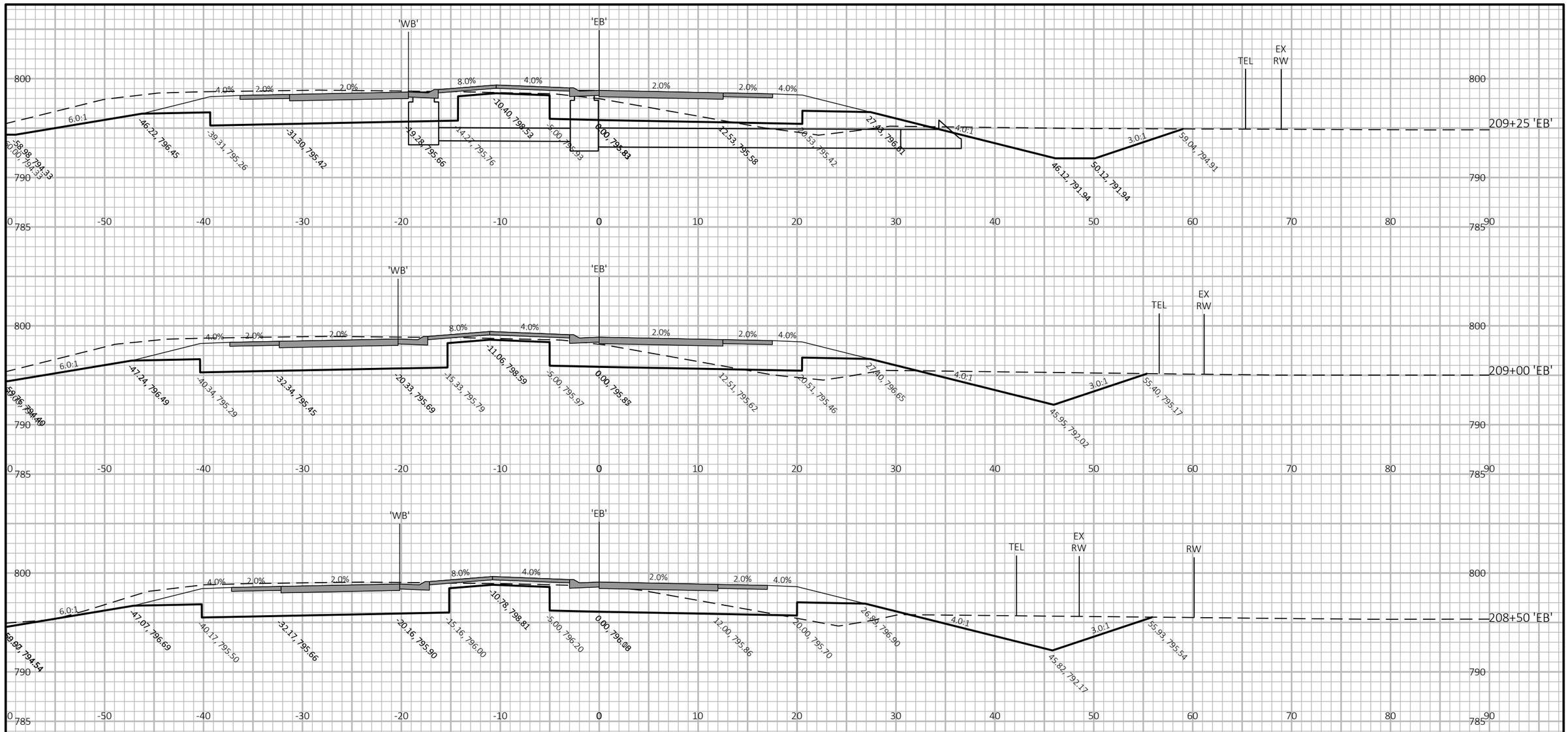
PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	CROSS SECTIONS: 'EB'	SHEET 176	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\090202-XS.DWG
LAYOUT NAME - EB2

PLOT DATE : 10/27/2023 2:55 PM PLOT BY : KUSCHEL, LEVI PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	CROSS SECTIONS: 'EB'	SHEET 177	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\090202-XS.DWG
LAYOUT NAME - EB3

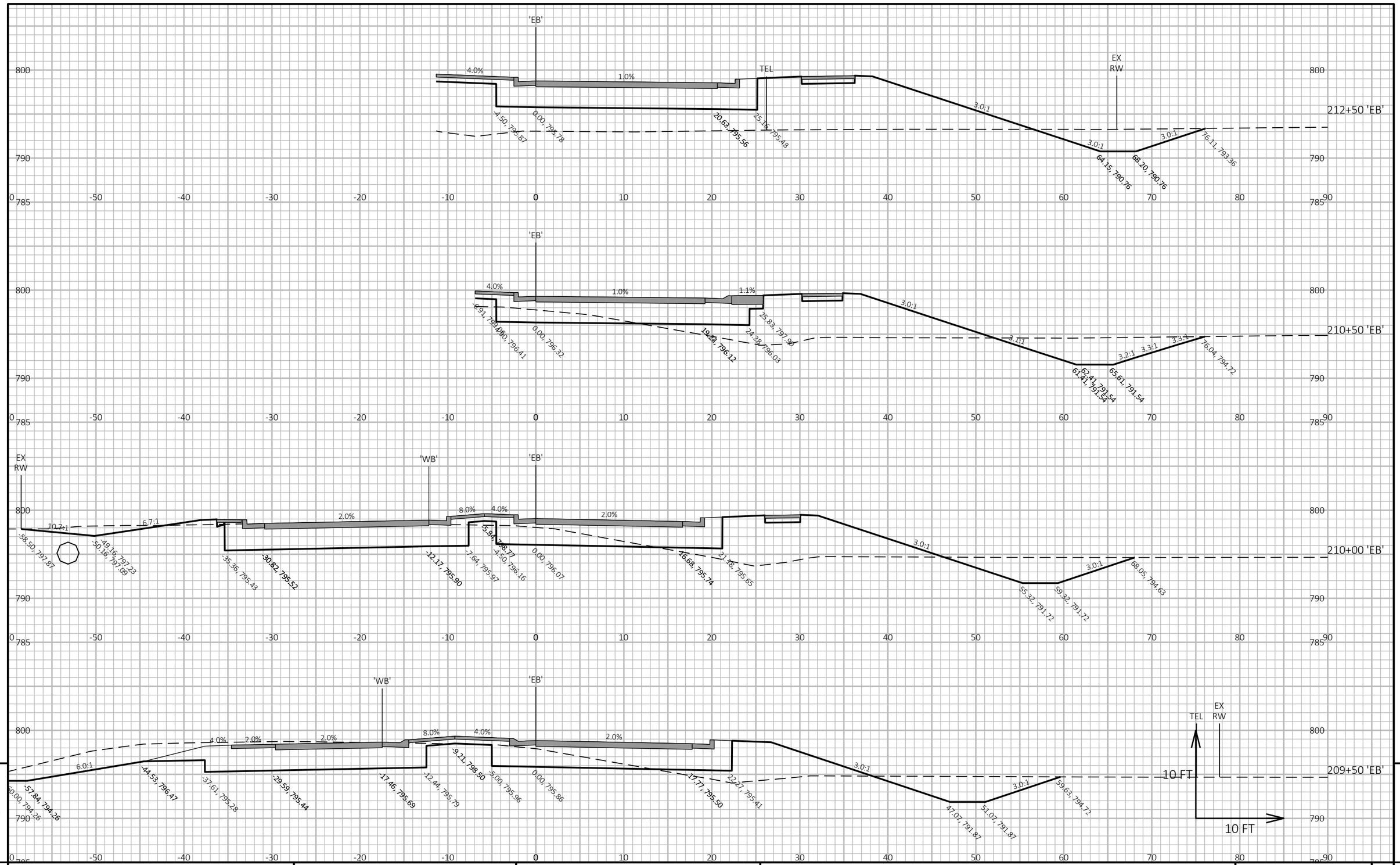
PLOT DATE : 10/27/2023 2:55 PM

PLOT BY : KUSCHEL, LEVI

PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	CROSS SECTIONS: 'EB'	SHEET 178 E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\090202-XS.DWG
LAYOUT NAME - EB4

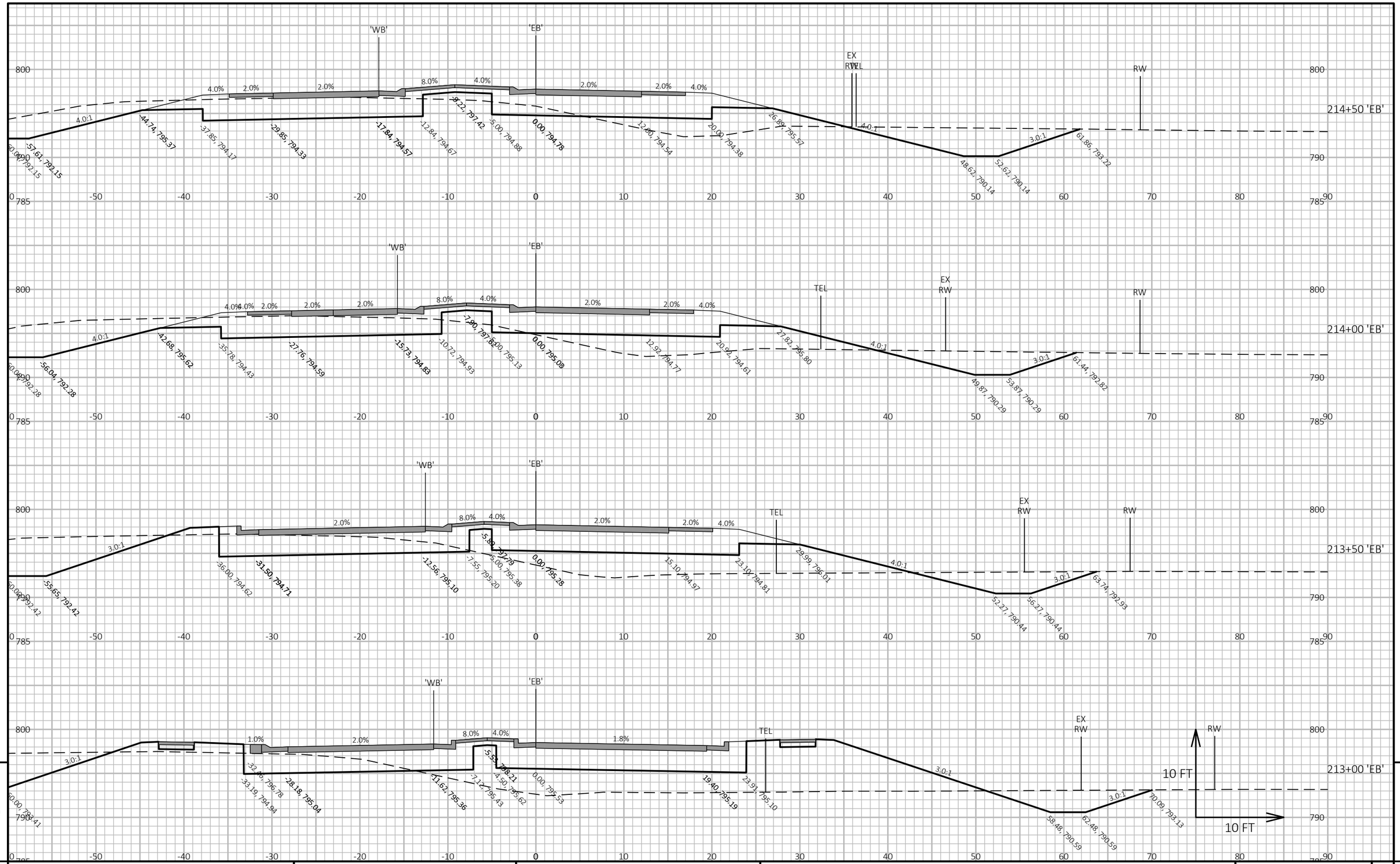
PLOT DATE : 10/27/2023 2:55 PM

PLOT BY : KUSCHEL, LEVI

PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	CROSS SECTIONS: 'EB'	SHEET 179	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\090202-XS.DWG
LAYOUT NAME - EB5

PLOT DATE : 10/27/2023 2:55 PM

PLOT BY : KUSCHEL, LEVI

PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49

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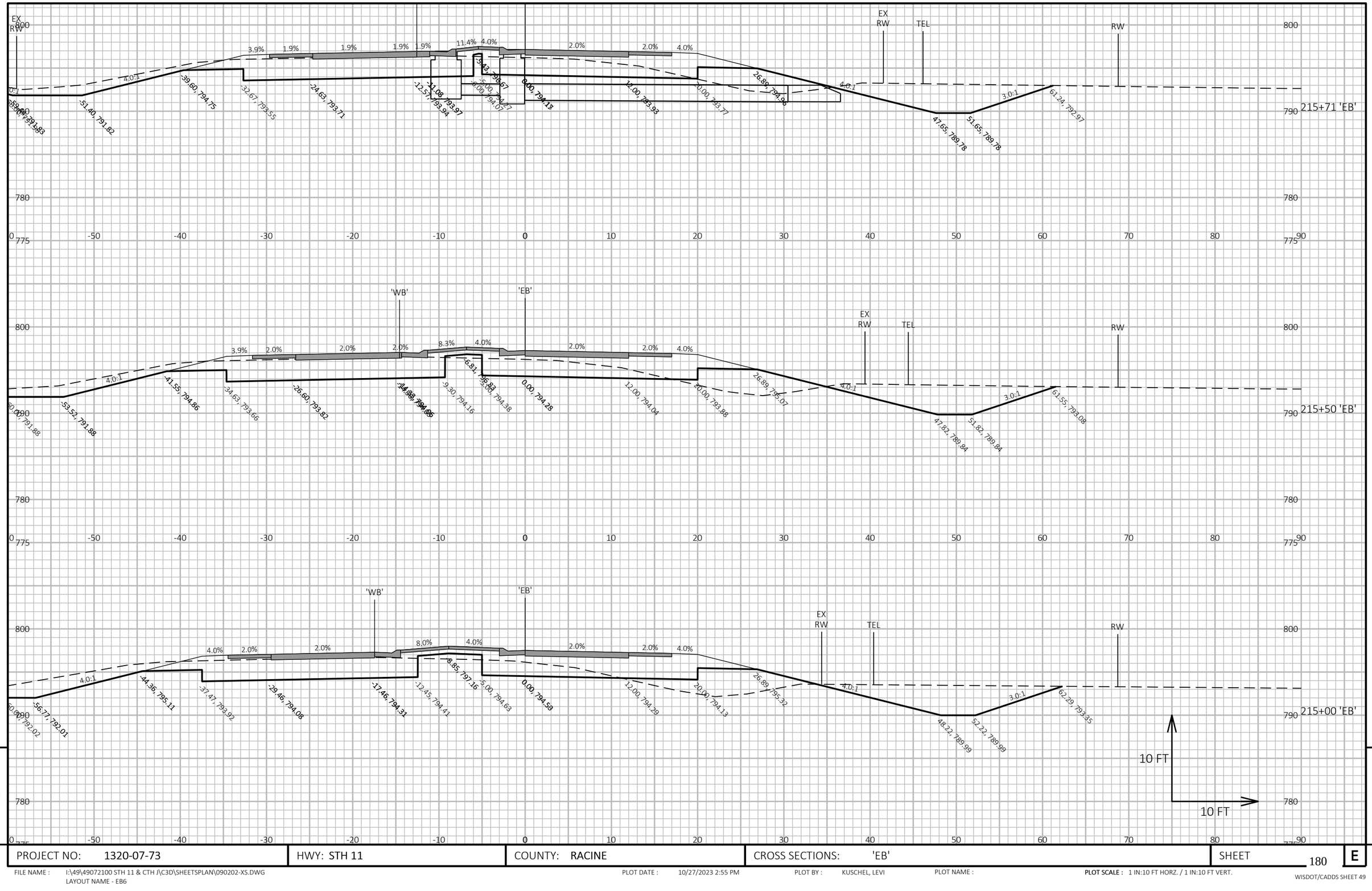
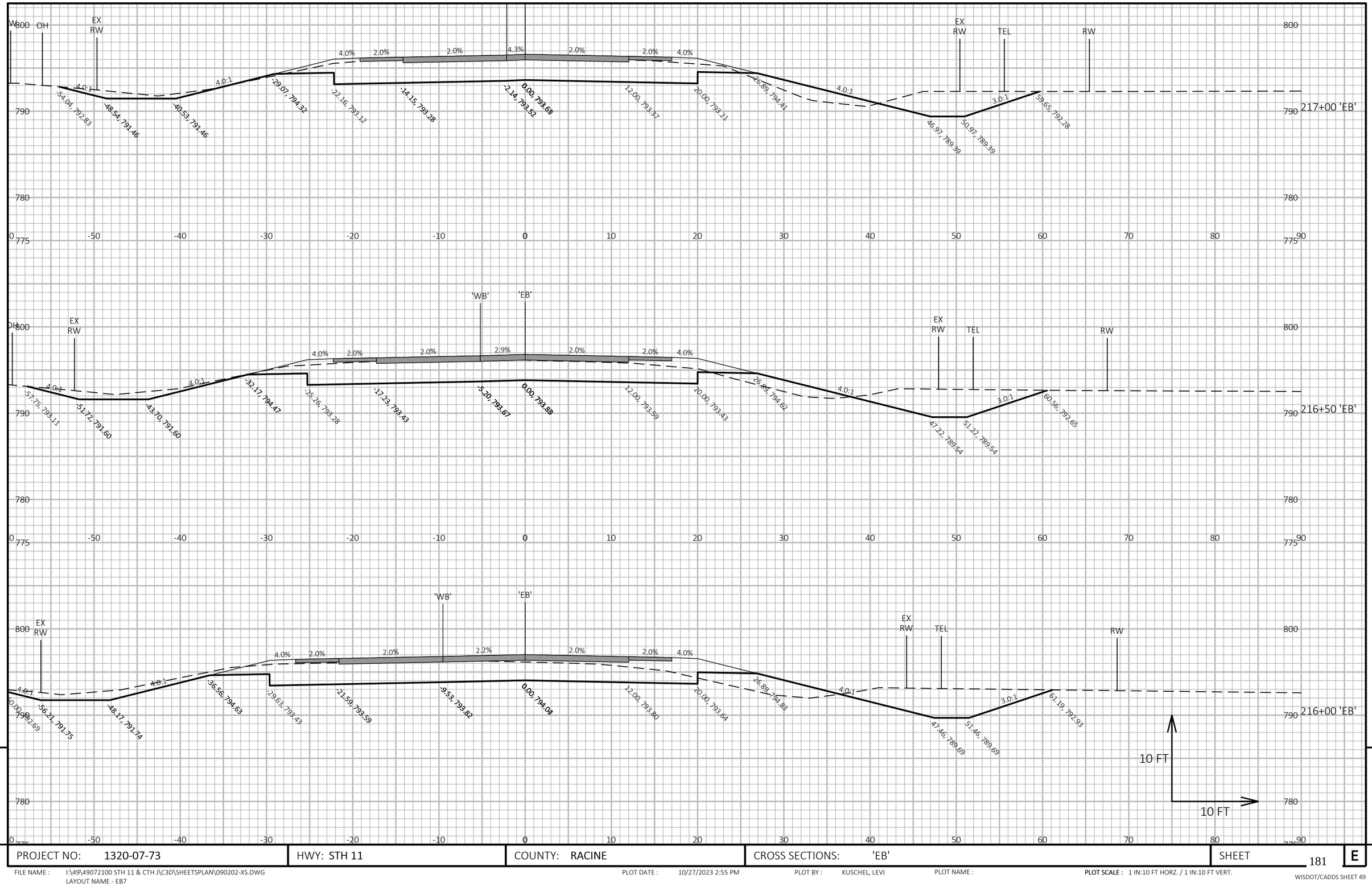


PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	CROSS SECTIONS: 'EB'	SHEET 180 E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\090202-XS.DWG
LAYOUT NAME - EB6

PLOT DATE : 10/27/2023 2:55 PM PLOT BY : KUSCHEL, LEVI PLOT NAME : PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



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PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	CROSS SECTIONS: 'EB'	SHEET 181	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\090202-XS.DWG
LAYOUT NAME - EB7

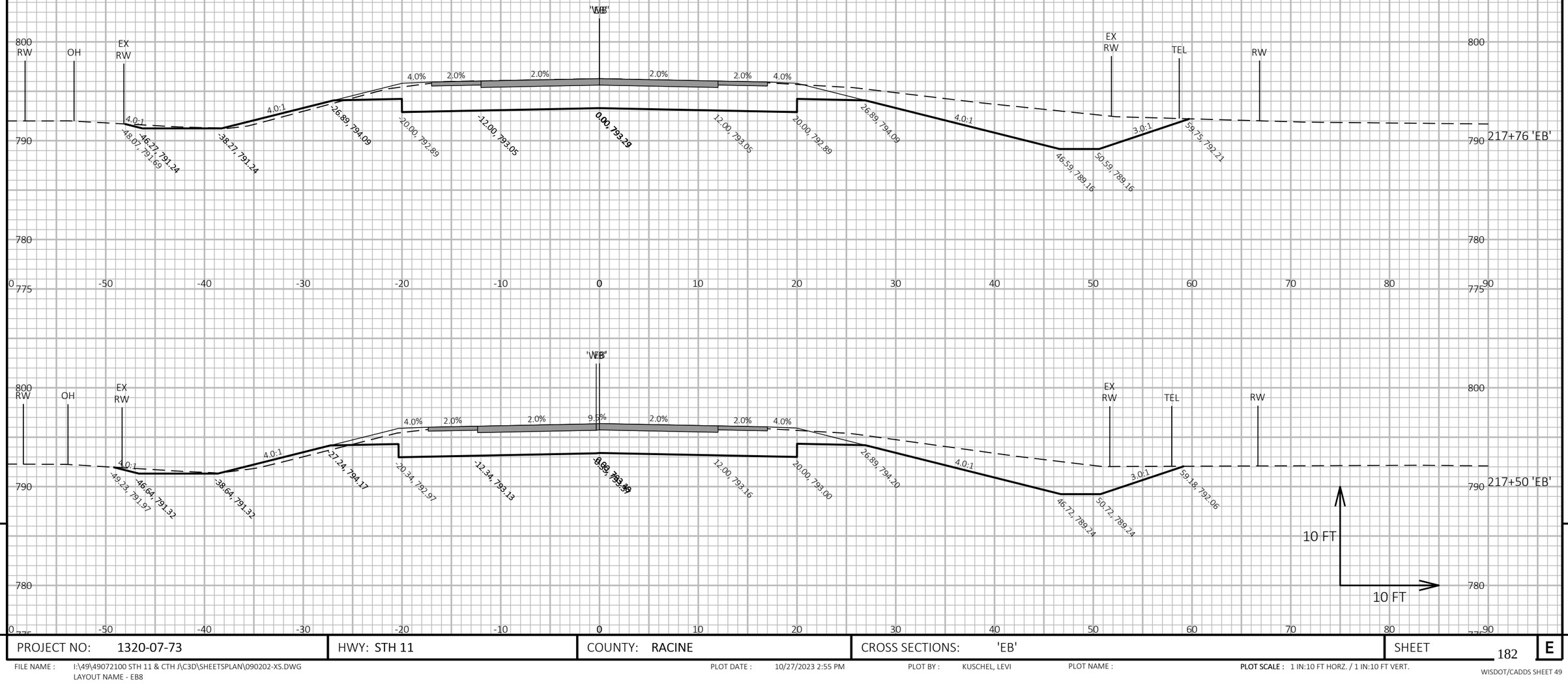
PLOT DATE : 10/27/2023 2:55 PM

PLOT BY : KUSCHEL, LEVI

PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	CROSS SECTIONS: 'EB'	SHEET 182 E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\090202-XS.DWG
LAYOUT NAME - EB8

PLOT DATE : 10/27/2023 2:55 PM

PLOT BY : KUSCHEL, LEVI

PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49

9

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PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	CROSS SECTIONS: 'WB'	SHEET 183	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\090202-XS.DWG
LAYOUT NAME - WB1

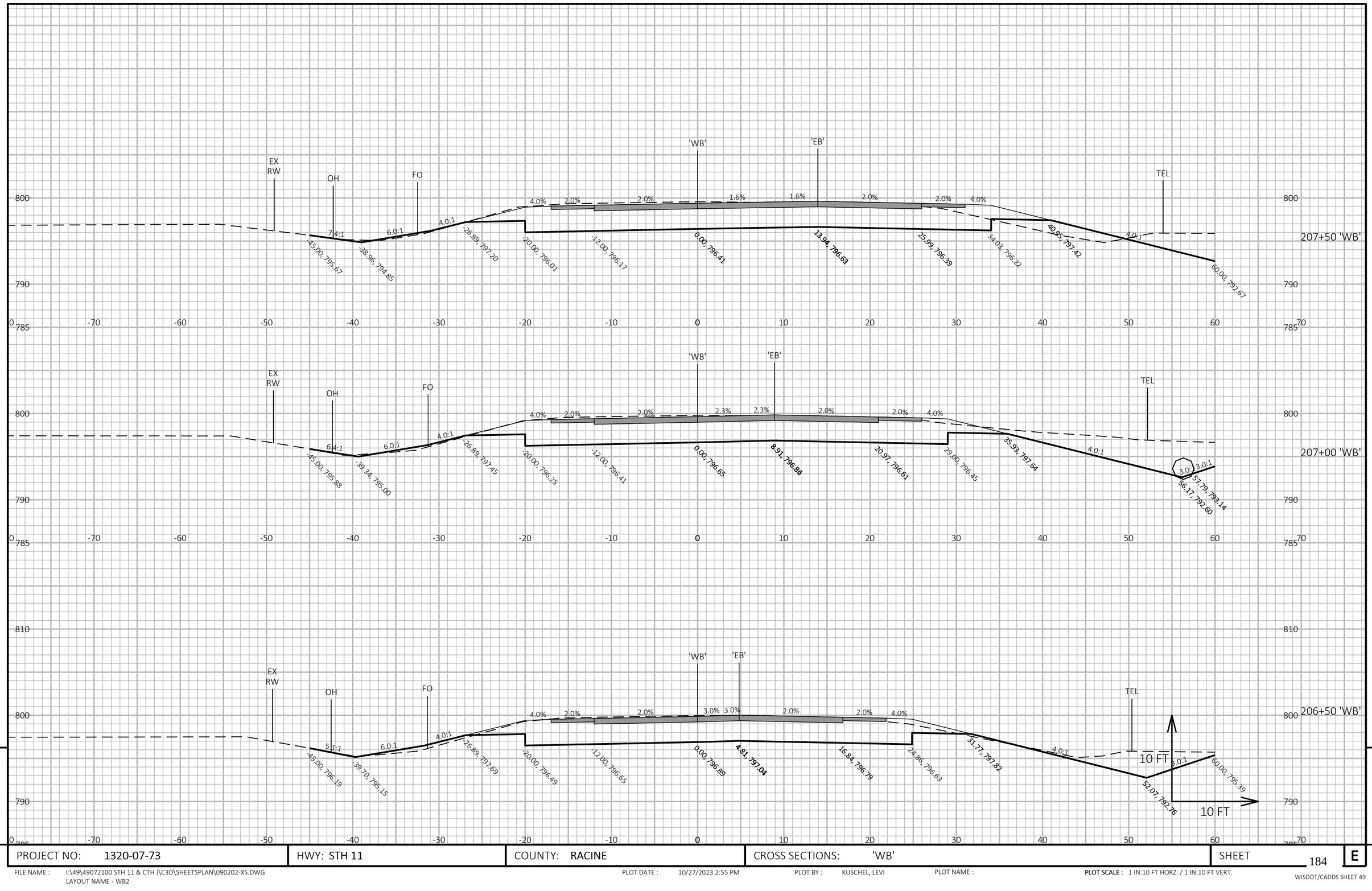
PLOT DATE : 10/27/2023 2:55 PM

PLOT BY : KUSCHEL, LEVI

PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



9

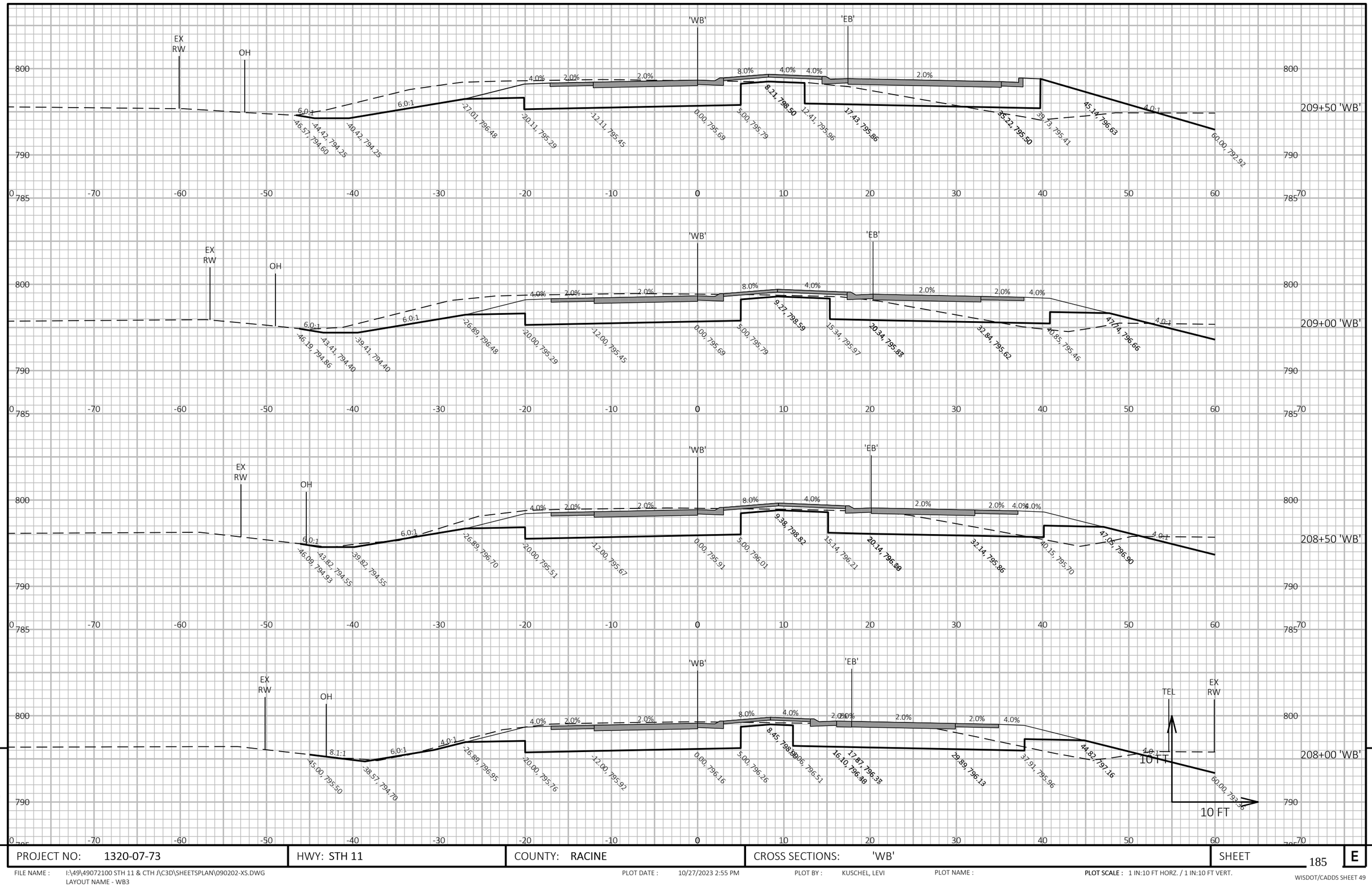
9

PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	CROSS SECTIONS: 'WB'	SHEET 184	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\090202-XS.DWG
LAYOUT NAME - WB2

PLOT DATE : 10/27/2023 2:55 PM PLOT BY : KUSCHEL, LEVI PLOT NAME : PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



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PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	CROSS SECTIONS: 'WB'	SHEET 185	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\090202-XS.DWG
LAYOUT NAME - WB3

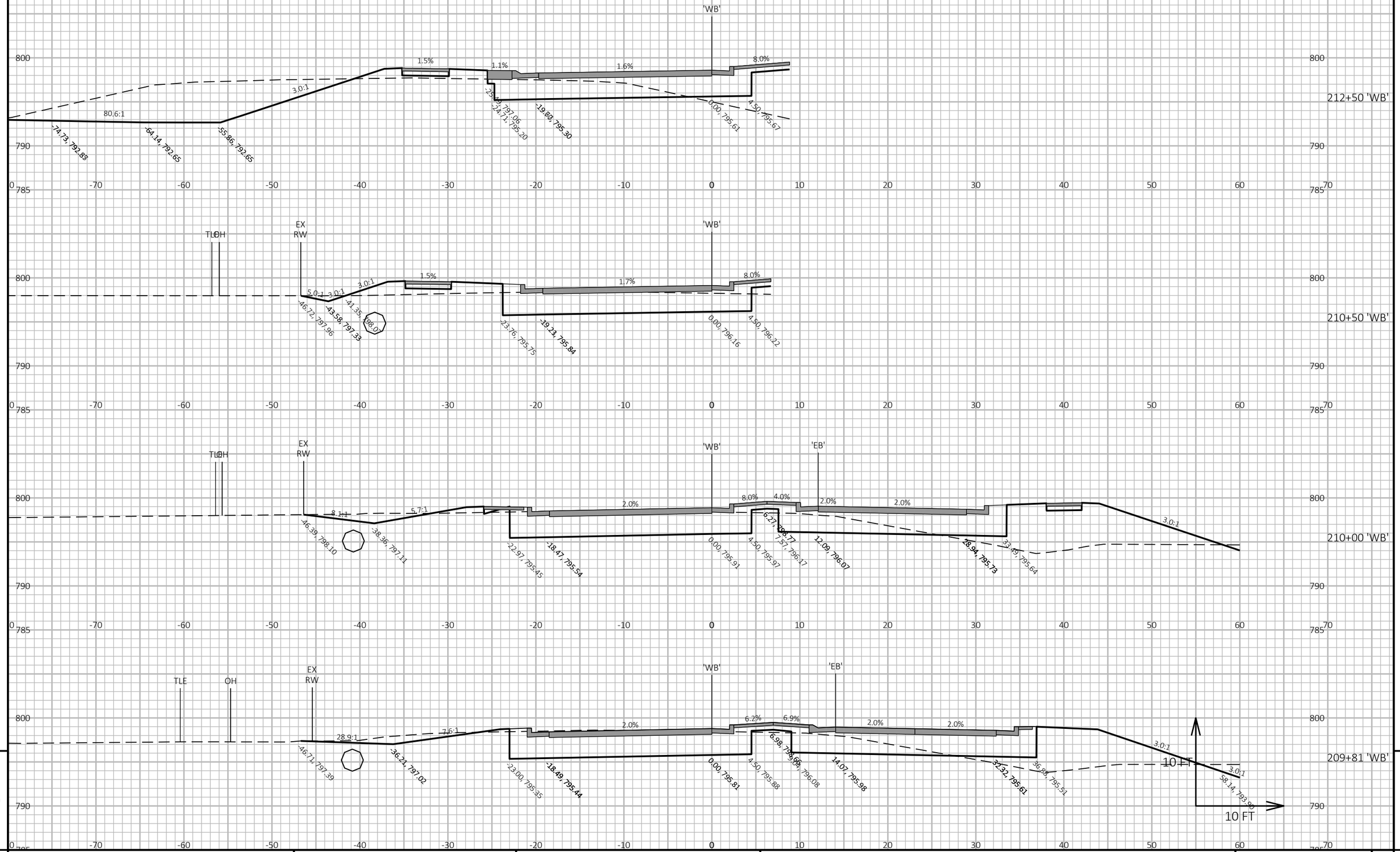
PLOT DATE : 10/27/2023 2:55 PM

PLOT BY : KUSCHEL, LEVI

PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



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9

PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	CROSS SECTIONS: 'WB'	SHEET 186	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\090202-XS.DWG
LAYOUT NAME - WB4

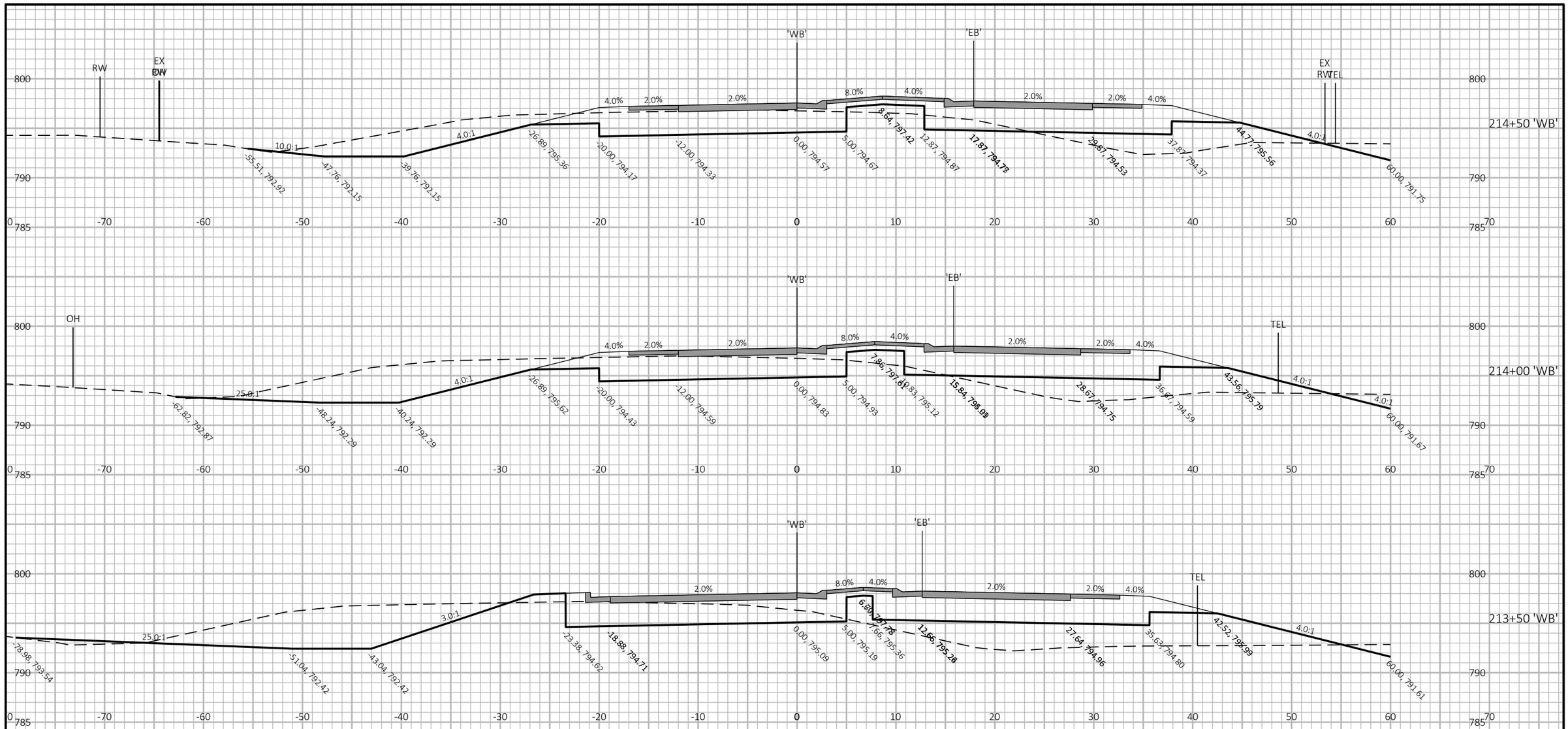
PLOT DATE : 10/27/2023 2:55 PM

PLOT BY : KUSCHEL, LEVI

PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



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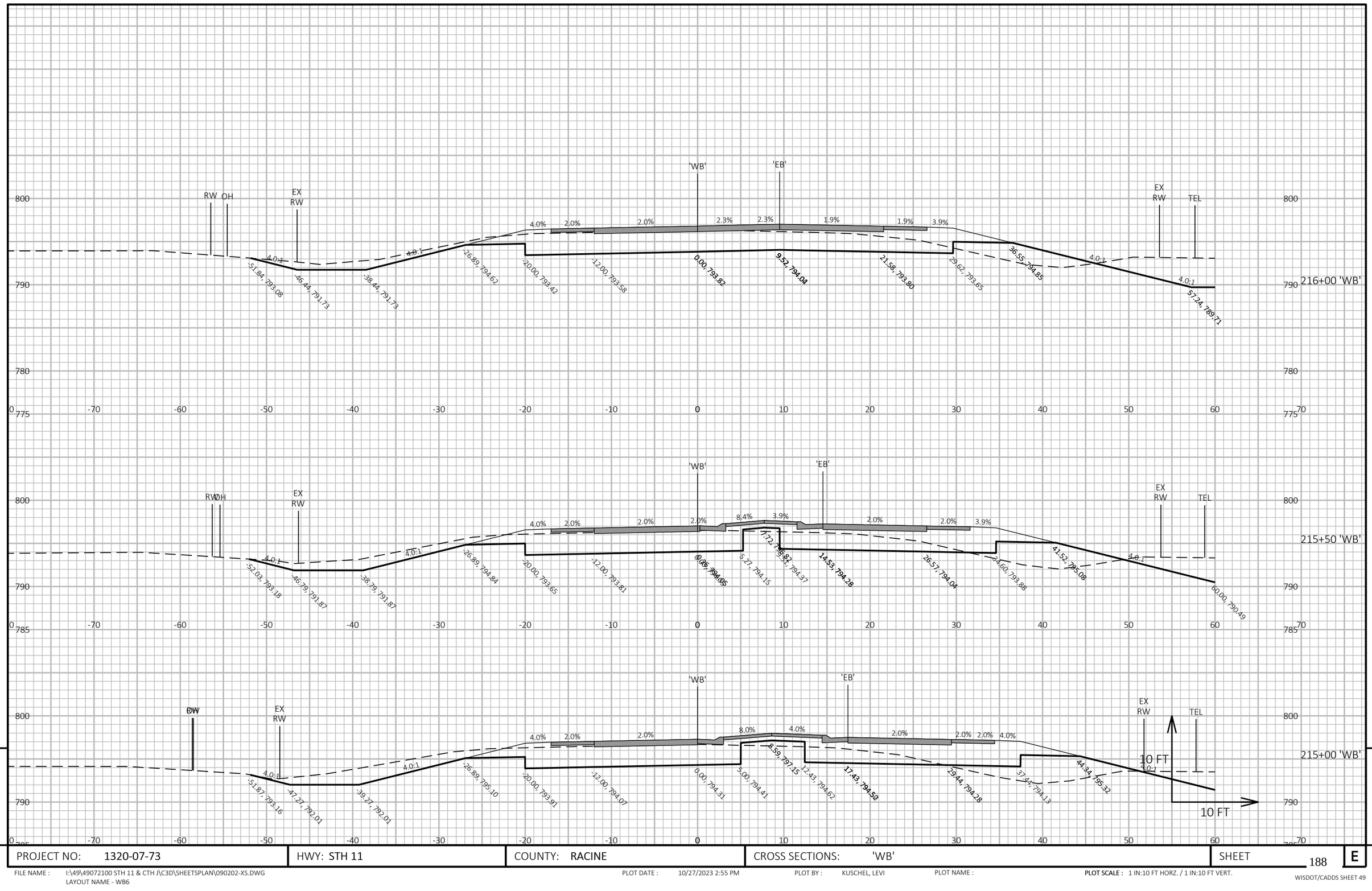
PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	CROSS SECTIONS: 'WB'	SHEET 187	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\090202-XS.DWG
LAYOUT NAME - WB5

PLOT DATE : 10/27/2023 2:55 PM PLOT BY : KUSCHEL, LEVI PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



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PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	CROSS SECTIONS: 'WB'	SHEET 188	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\090202-XS.DWG
LAYOUT NAME - WB6

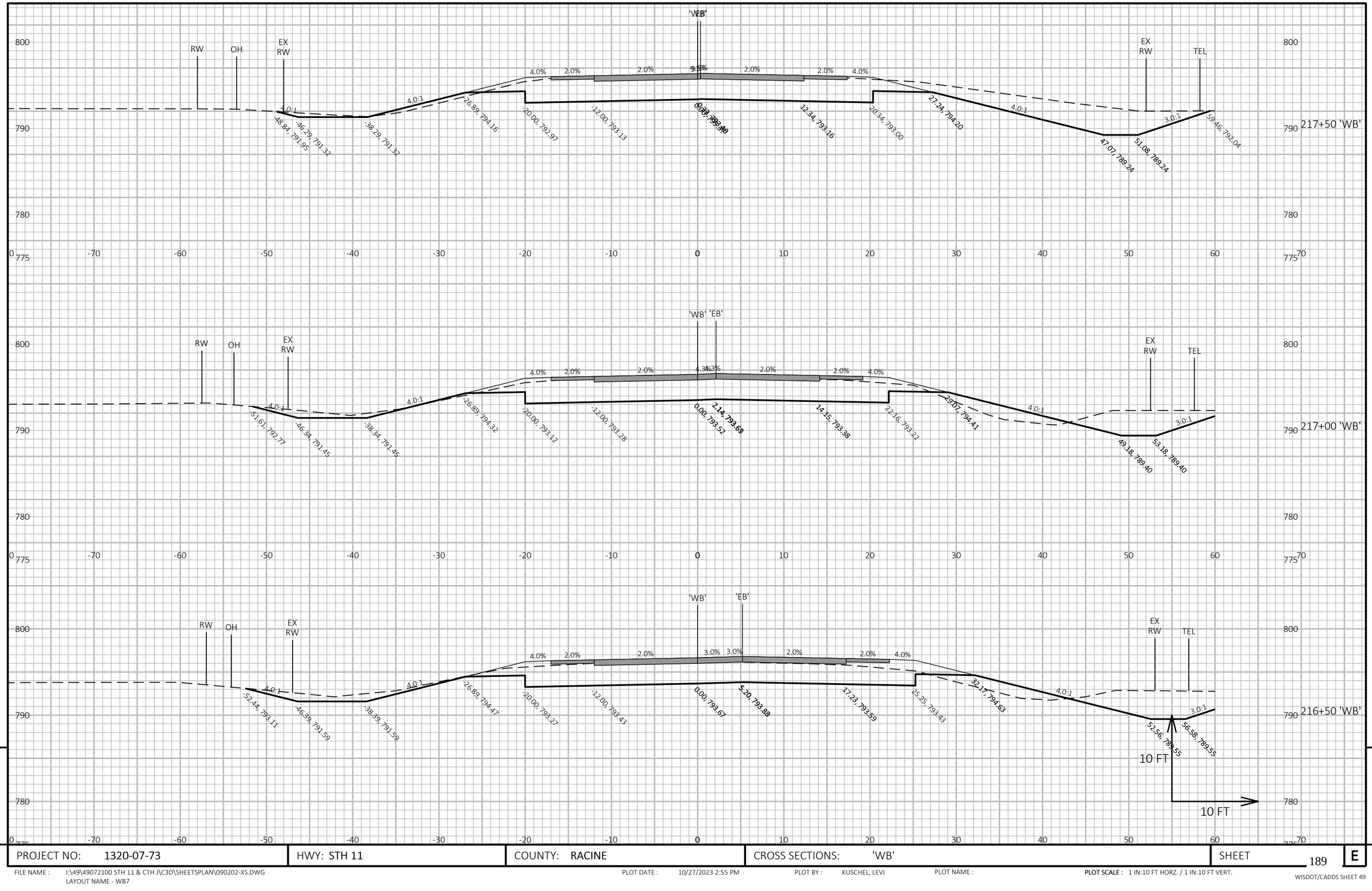
PLOT DATE : 10/27/2023 2:55 PM

PLOT BY : KUSCHEL, LEVI

PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	CROSS SECTIONS: 'WB'	SHEET 189 E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\090202-XS.DWG
LAYOUT NAME - WB7

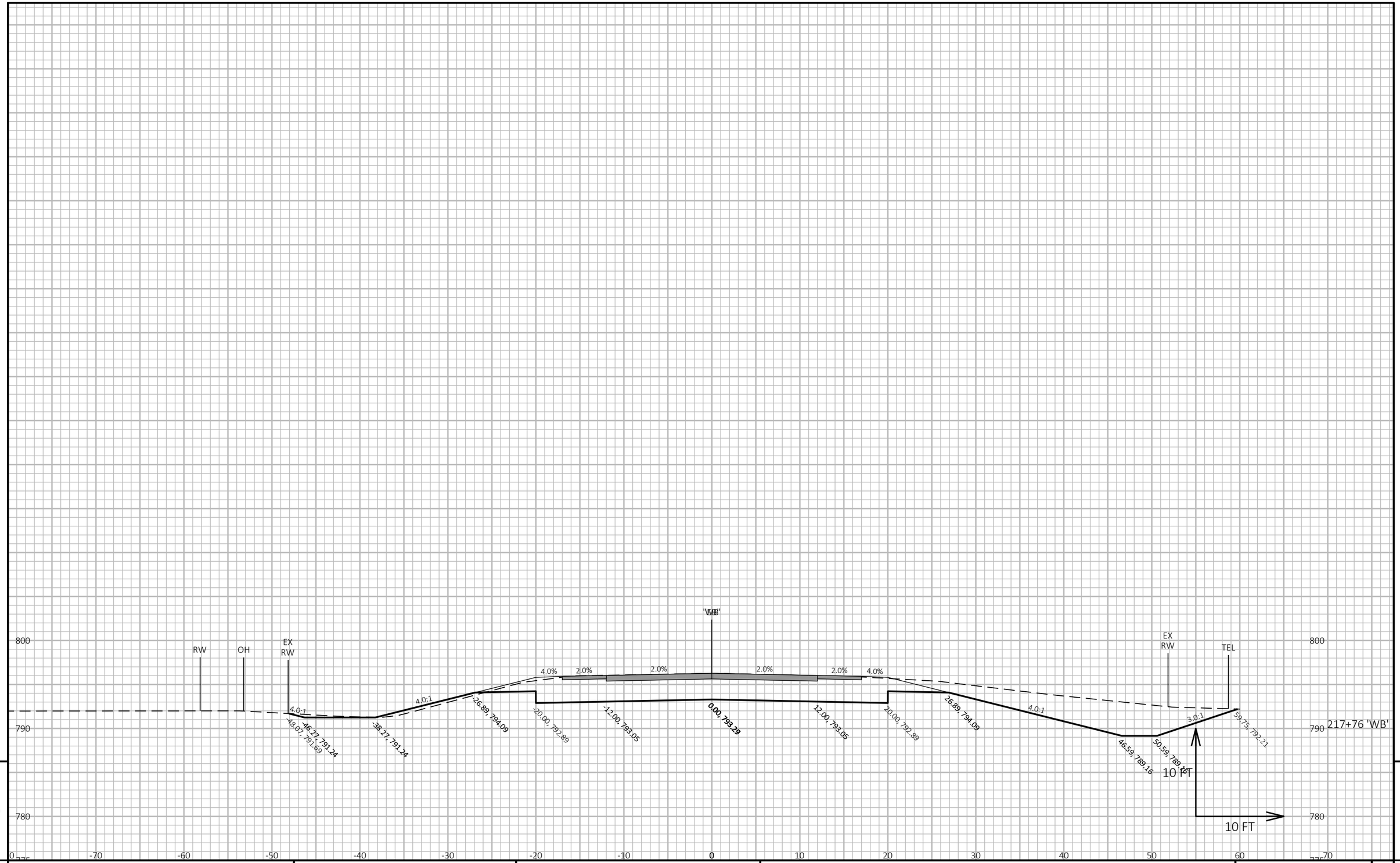
PLOT DATE : 10/27/2023 2:55 PM

PLOT BY : KUSCHEL, LEVI

PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



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9

PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	CROSS SECTIONS: 'WB'	SHEET 190	E

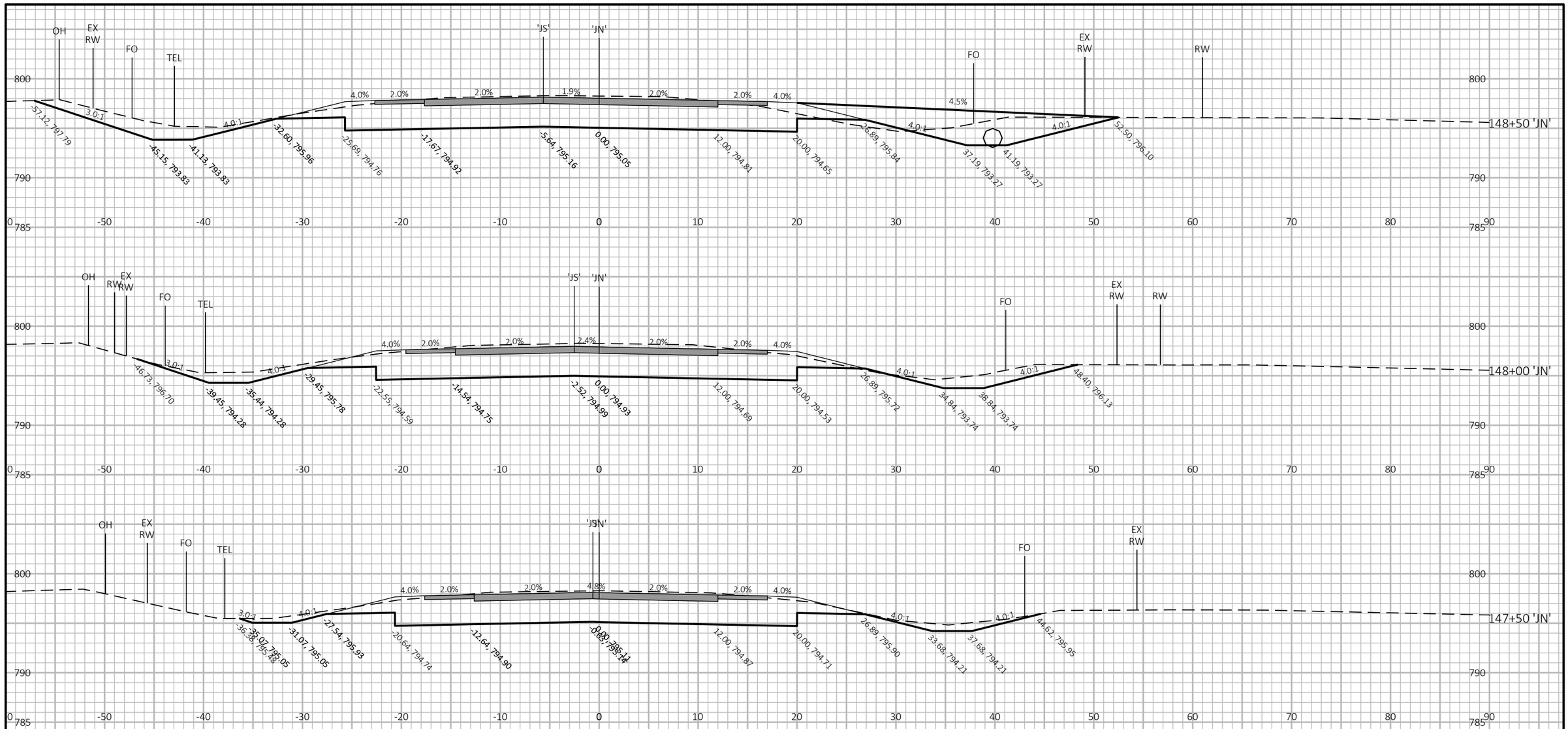
FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\090202-XS.DWG
LAYOUT NAME - WB8

PLOT DATE : 10/27/2023 2:55 PM

PLOT BY : KUSCHEL, LEVI

PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.



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PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	CROSS SECTIONS: 'JN'	SHEET 191	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\090202-XS.DWG
LAYOUT NAME - JN1

PLOT DATE : 10/27/2023 2:55 PM

PLOT BY : KUSCHEL, LEVI

PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49

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9

PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	CROSS SECTIONS: 'JN'	SHEET 192	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\090202-XS.DWG
LAYOUT NAME - JN2

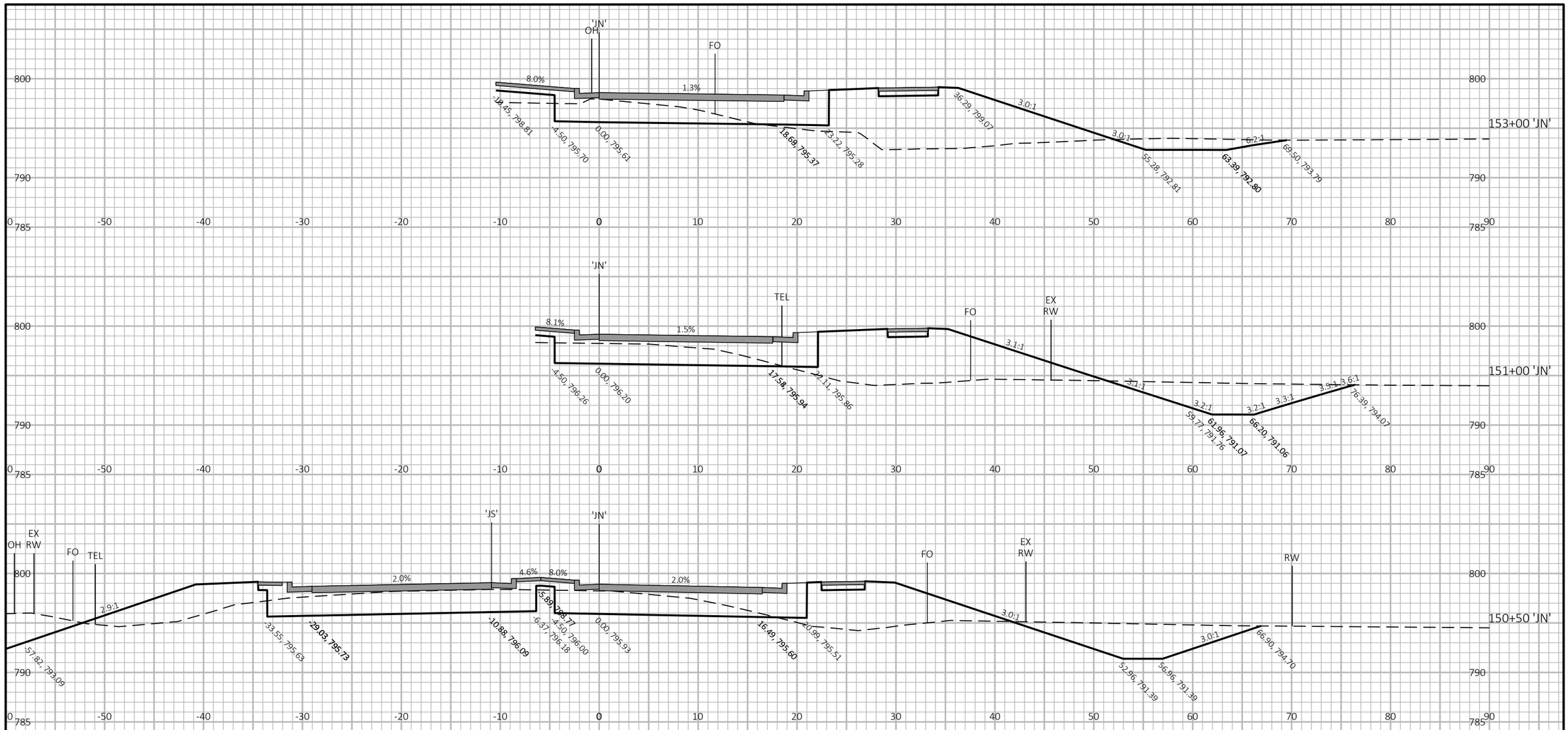
PLOT DATE : 10/27/2023 2:55 PM

PLOT BY : KUSCHEL, LEVI

PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



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PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	CROSS SECTIONS: 'JN'	SHEET 193	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\090202-XS.DWG
LAYOUT NAME - JN3

PLOT DATE : 10/27/2023 2:55 PM

PLOT BY : KUSCHEL, LEVI

PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49

9

9

PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	CROSS SECTIONS: 'JN'	SHEET 194	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\090202-XS.DWG
LAYOUT NAME - JN4

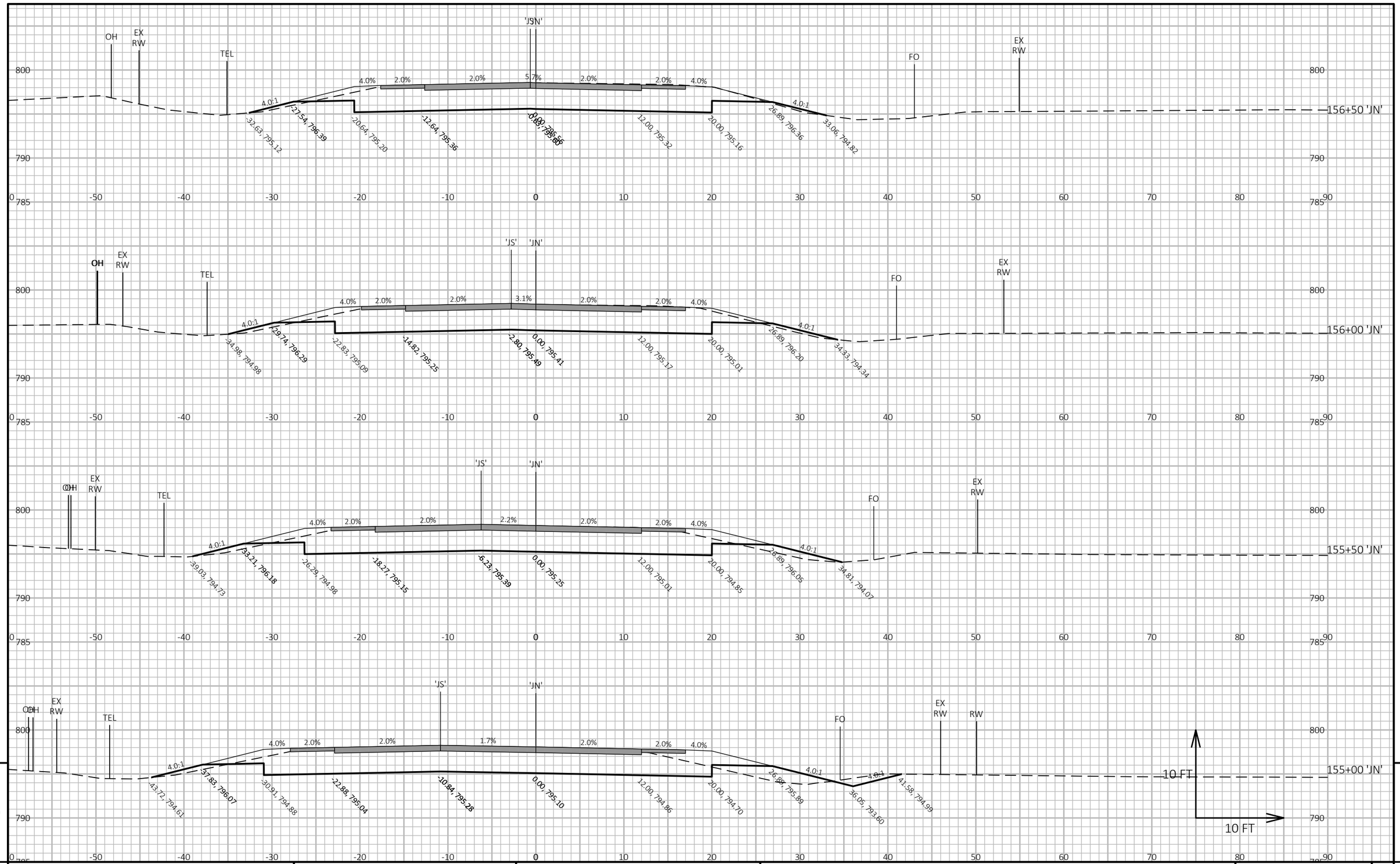
PLOT DATE : 10/27/2023 2:55 PM

PLOT BY : KUSCHEL, LEVI

PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



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PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	CROSS SECTIONS: 'JN'	SHEET 195	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\090202-XS.DWG
LAYOUT NAME - JN5

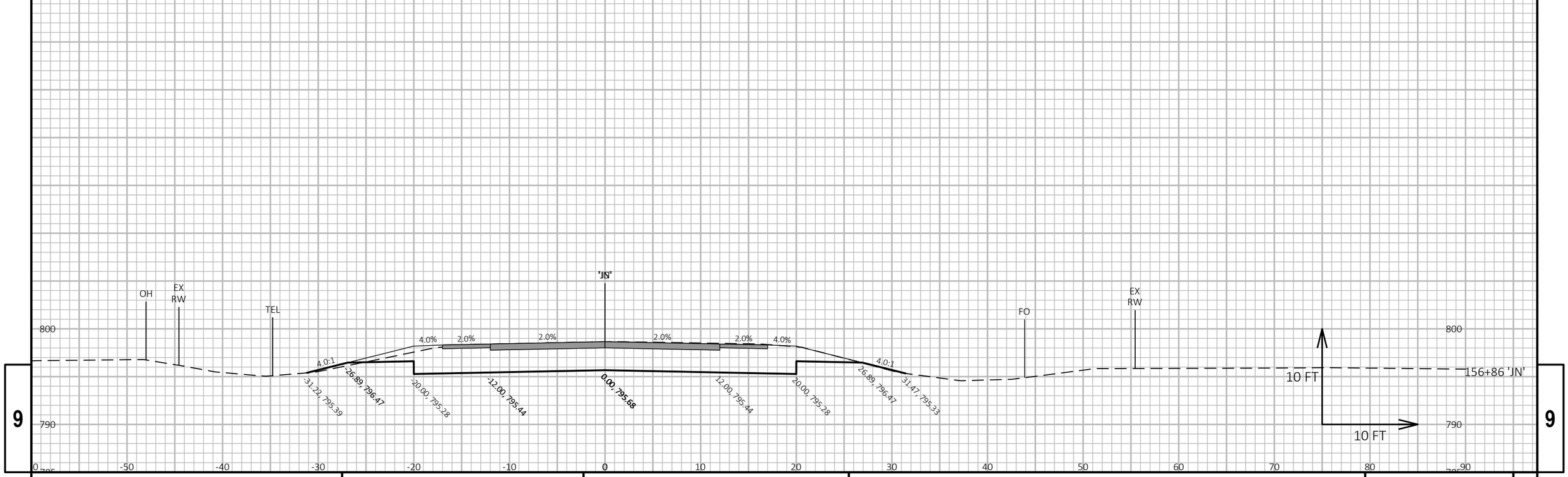
PLOT DATE : 10/27/2023 2:55 PM

PLOT BY : KUSCHEL, LEVI

PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



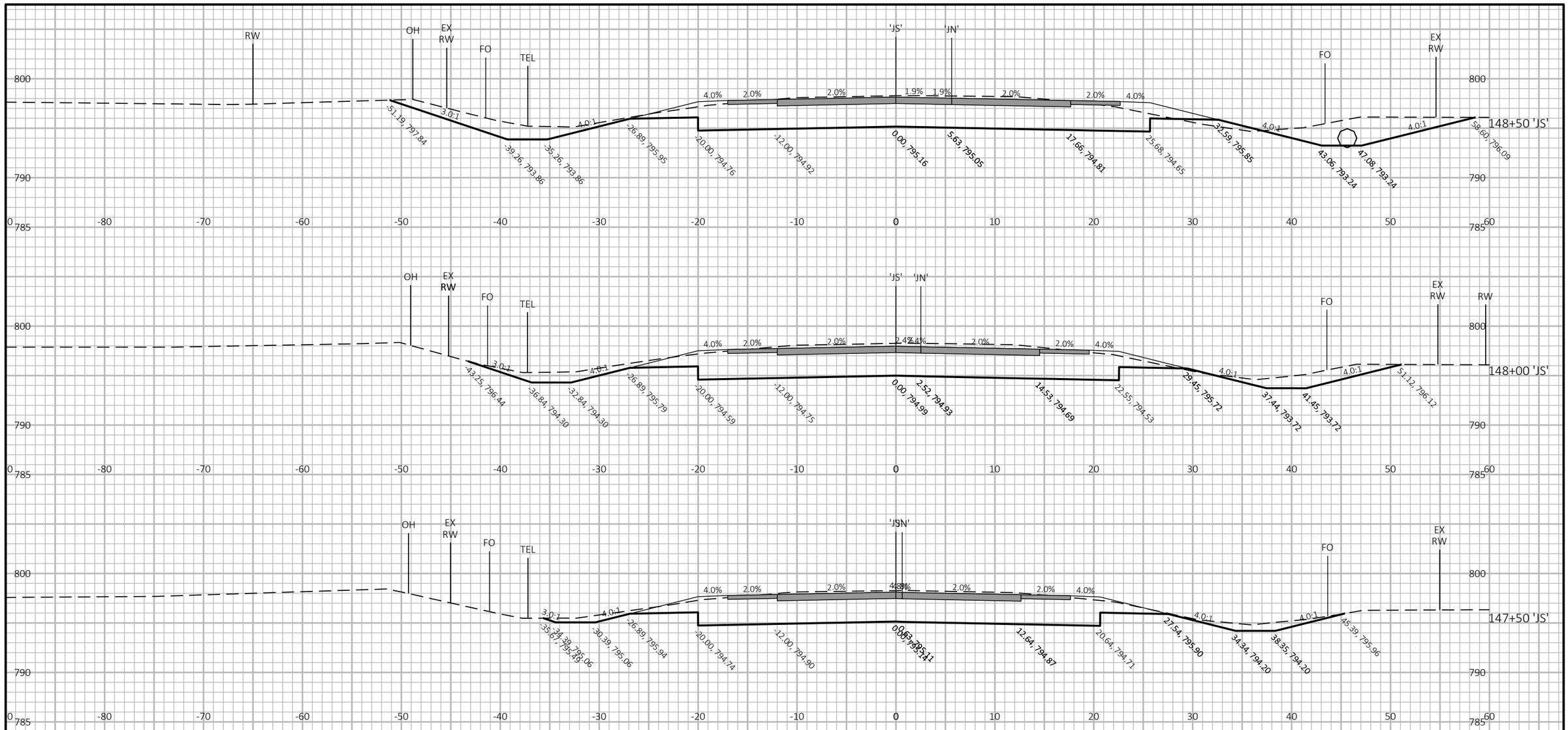
9 9

PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	CROSS SECTIONS: 'JN'	SHEET 196	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\090202-XS.DWG
LAYOUT NAME - JN6

PLOT DATE : 10/27/2023 2:55 PM PLOT BY : KUSCHEL, LEVI PLOT NAME : **PLOT SCALE :** 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



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PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	CROSS SECTIONS: 'JS'	SHEET 197	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\090202-XS.DWG
LAYOUT NAME - JS1

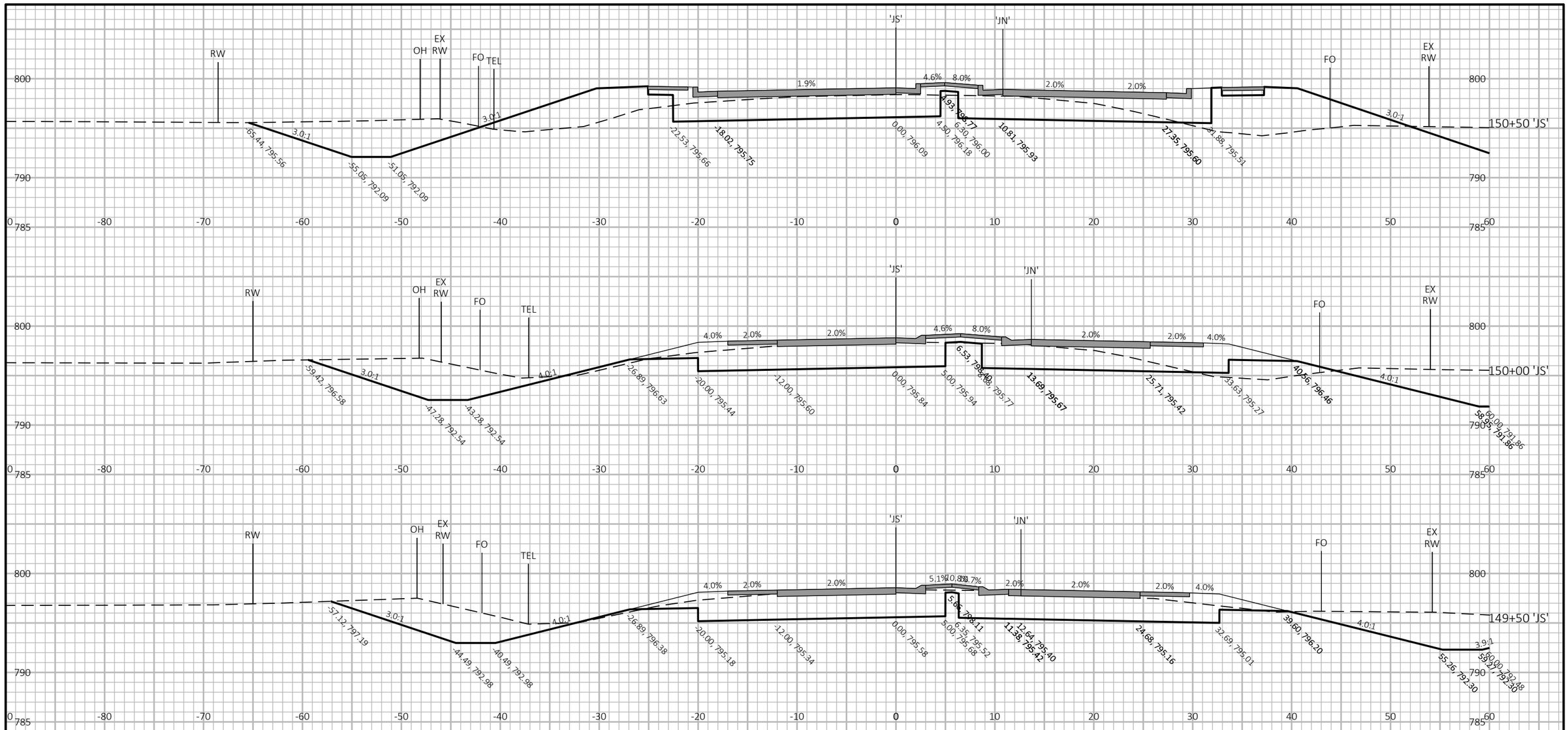
PLOT DATE : 10/27/2023 2:55 PM

PLOT BY : KUSCHEL, LEVI

PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	CROSS SECTIONS: 'JS'	SHEET 198	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\090202-XS.DWG
LAYOUT NAME - JS2

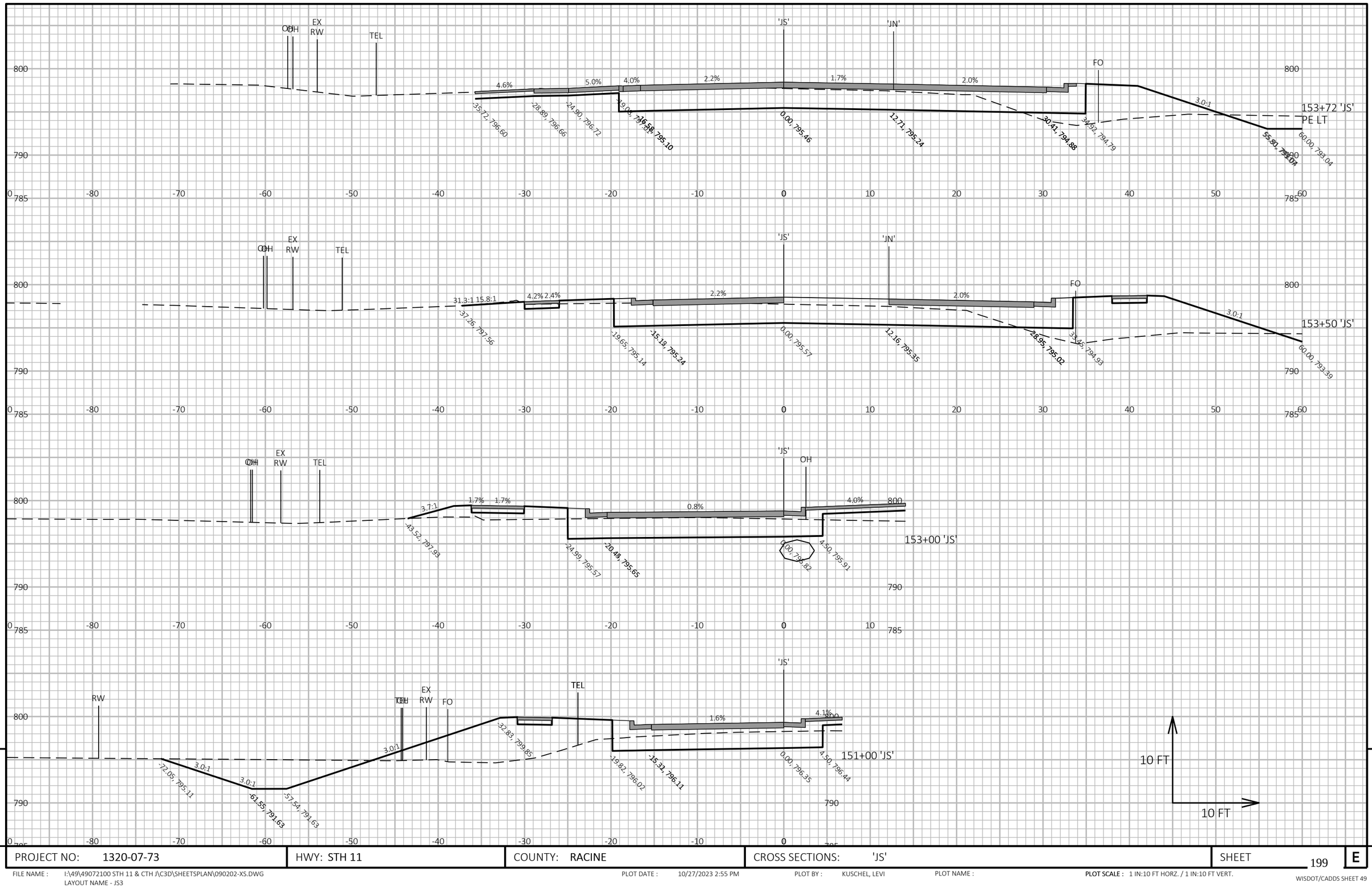
PLOT DATE : 10/27/2023 2:55 PM

PLOT BY : KUSCHEL, LEVI

PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



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PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	CROSS SECTIONS: 'JS'	SHEET 199	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\090202-XS.DWG
LAYOUT NAME - JS3

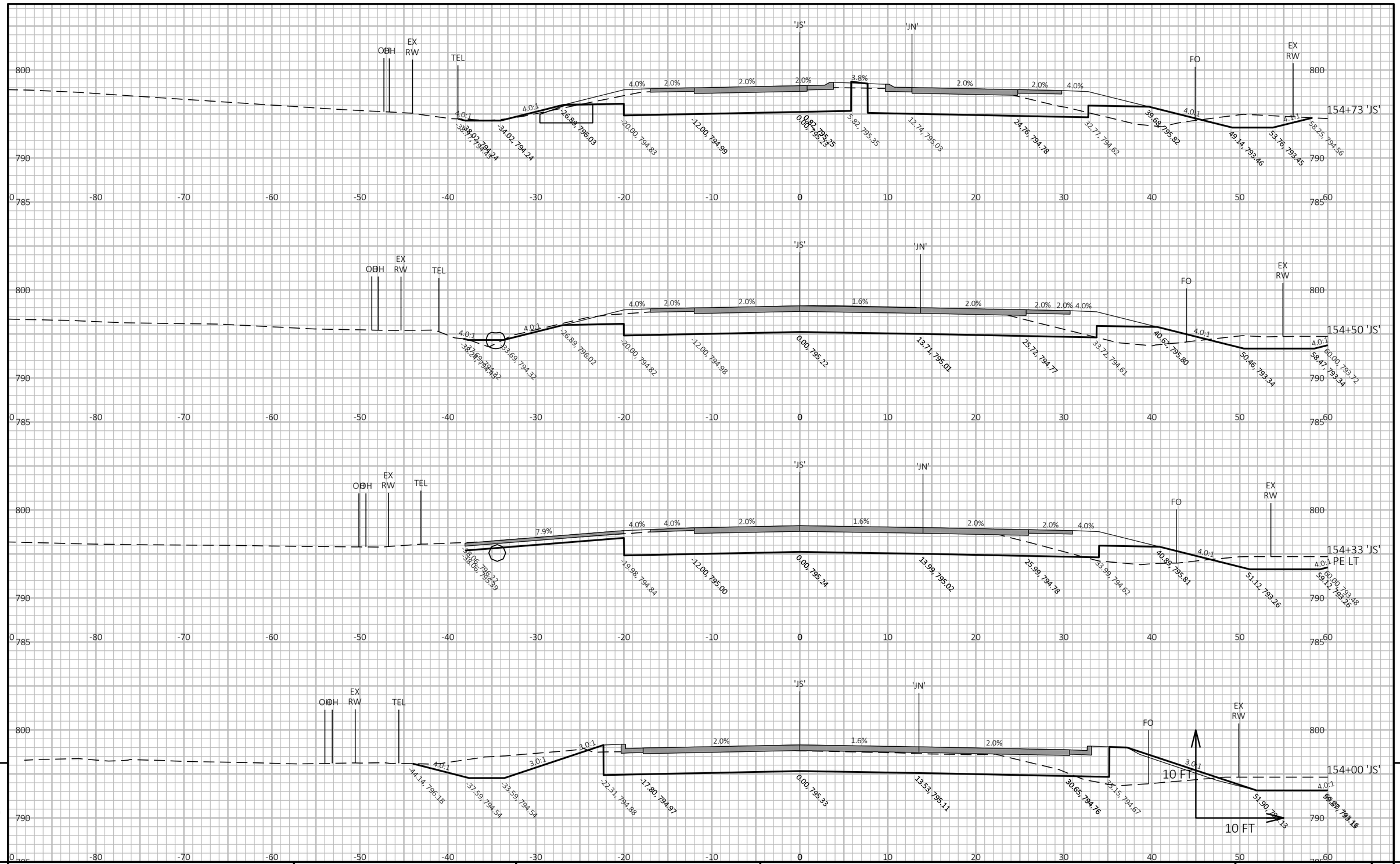
PLOT DATE : 10/27/2023 2:55 PM

PLOT BY : KUSCHEL, LEVI

PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



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PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	CROSS SECTIONS: 'JS'	SHEET 200	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\090202-XS.DWG
LAYOUT NAME - JS4

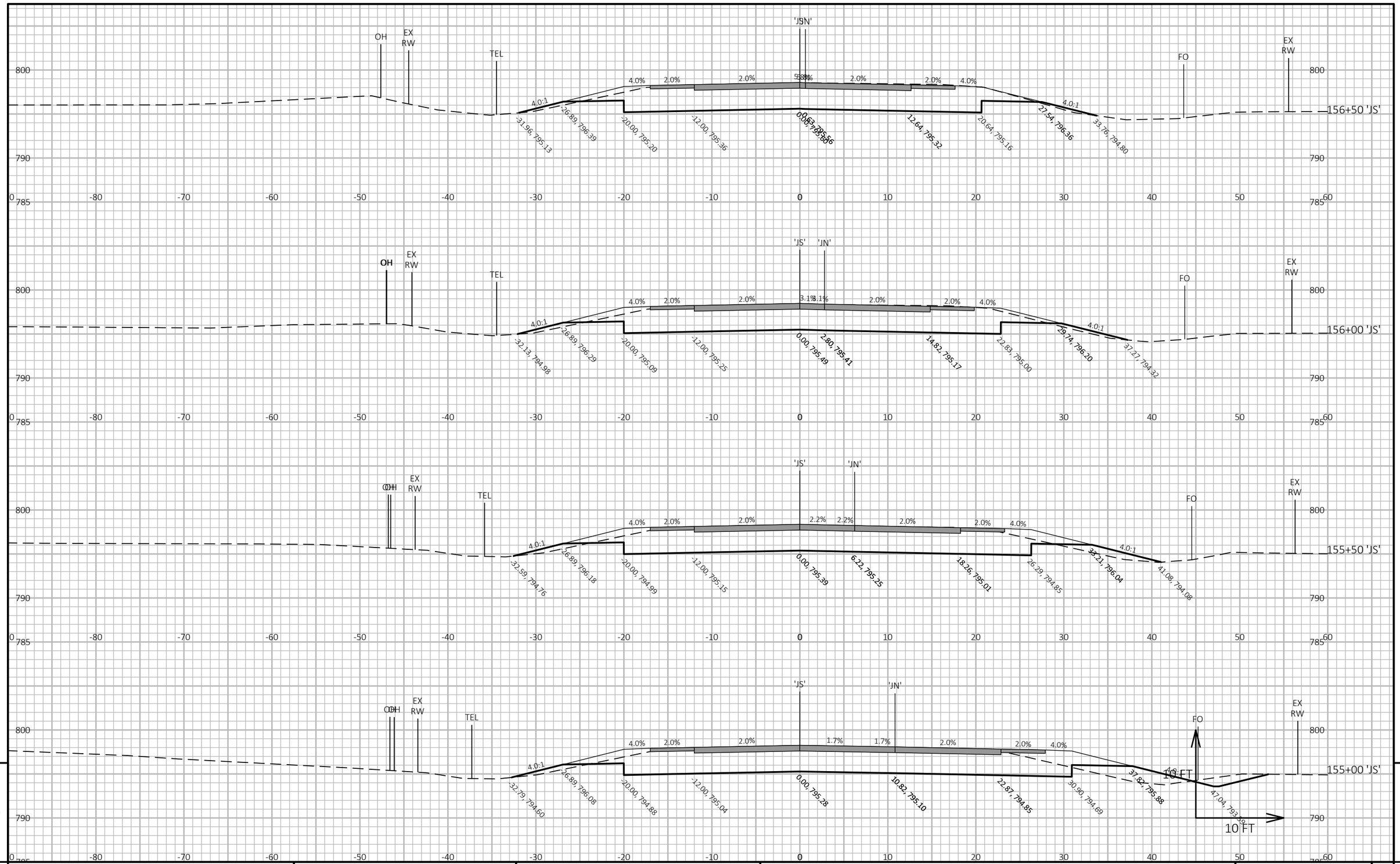
PLOT DATE : 10/27/2023 2:55 PM

PLOT BY : KUSCHEL, LEVI

PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



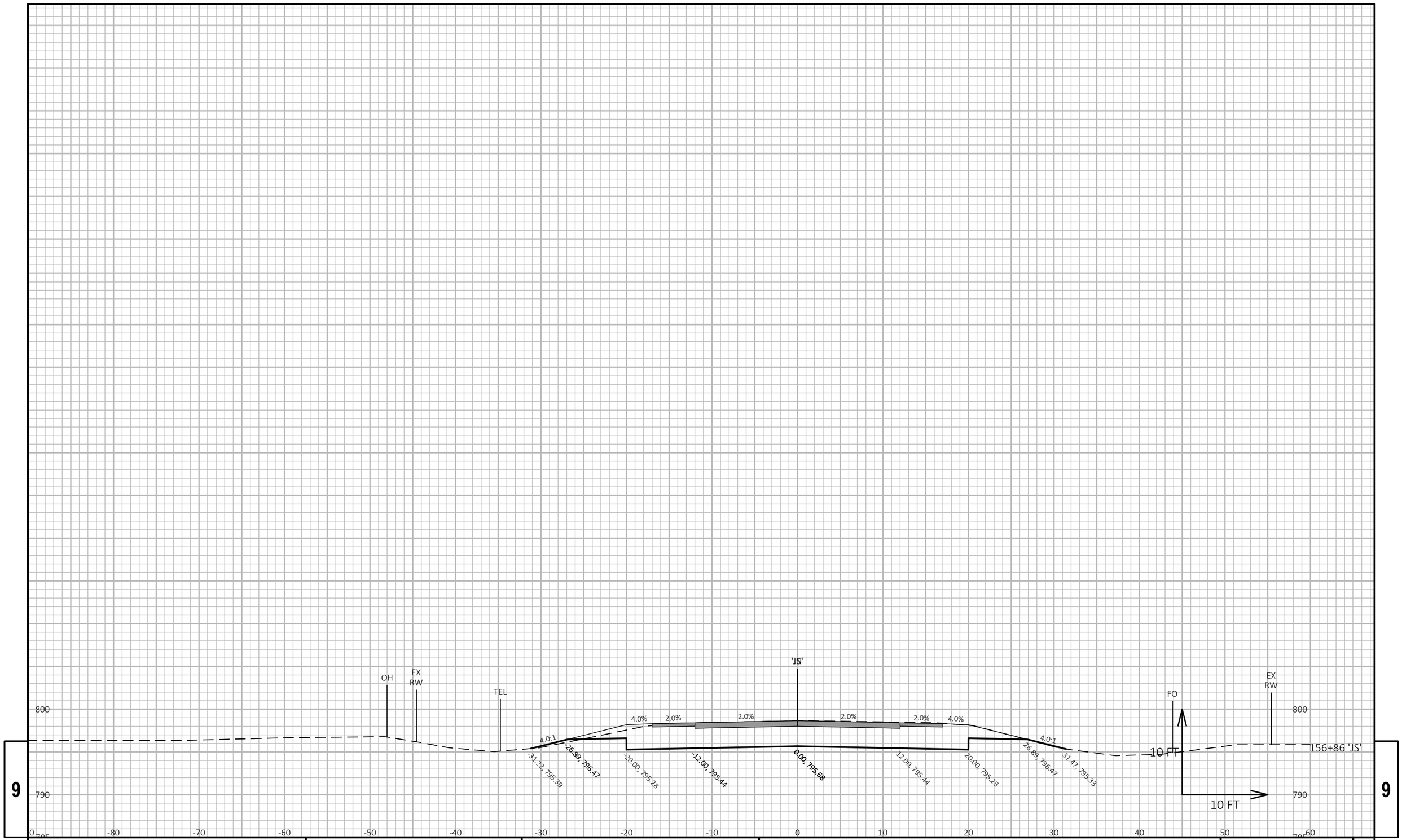
9 9

PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	CROSS SECTIONS: 'JS'	SHEET 201	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\090202-XS.DWG
LAYOUT NAME - JS5

PLOT DATE : 10/27/2023 2:56 PM PLOT BY : KUSCHEL, LEVI PLOT NAME : **PLOT SCALE :** 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



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PROJECT NO: 1320-07-73	HWY: STH 11	COUNTY: RACINE	CROSS SECTIONS: 'JS'	SHEET 202	E

FILE NAME : I:\49\49072100 STH 11 & CTH J\C3D\SHEETSPLAN\090202-XS.DWG
LAYOUT NAME - JS6

PLOT DATE : 10/27/2023 2:56 PM PLOT BY : KUSCHEL, LEVI PLOT NAME : **PLOT SCALE :** 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

Notes

Wisconsin Department of Transportation

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