

EAU PROJECT ID: 7995-02-63 WITH: N/A COUNTY: EAU CLAIRE

MARCH 2024

ORDER OF SHEETS

Section No.	1	Title
Section No.	2	Typical Sections and Details
Section No.	3	Estimate of Quantities
Section No.	3	Miscellaneous Quantities
Section No.	4	Right of Way Plat
Section No.	5	Plan and Profile
Section No.	6	Standard Detail Drawings
Section No.	7	Sign Plates
Section No.	8	<del>Structure Plans</del>
Section No.	9	Computer Earthwork Data
Section No.	9	Cross Sections

TOTAL SHEETS = 148



33

DESIGN DESIGNATION 7995-02-63

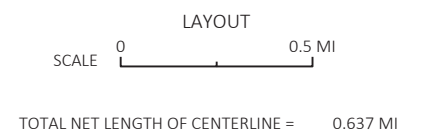
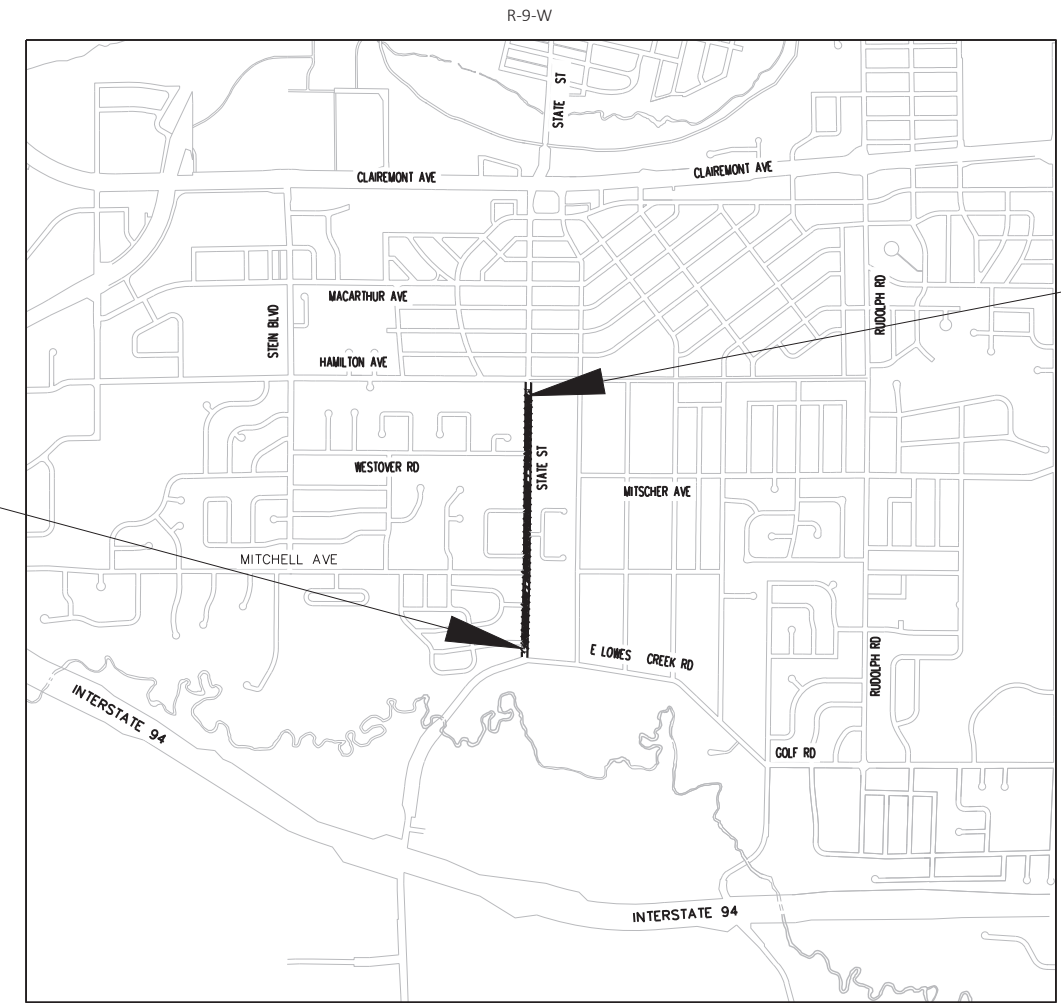
A.A.D.T.	2024	=	5,750
A.A.D.T.	2044	=	7,750
D.H.V.		=	575
D.D.		=	50/50
T.		=	9.5%
DESIGN SPEED		=	35 MPH
ESALS		=	2,000,000

CONVENTIONAL SYMBOLS

PLAN	PROFILE
CORPORATE LIMITS	GRADE LINE
PROPERTY LINE	ORIGINAL GROUND
LOT LINE	MARSH OR ROCK PROFILE (To be noted as such)
LIMITED HIGHWAY EASEMENT	SPECIAL DITCH
EXISTING RIGHT OF WAY	GRADE ELEVATION
PROPOSED OR NEW R/W LINE	CULVERT (Profile View)
SLOPE INTERCEPT	UTILITIES
REFERENCE LINE	ELECTRIC
EXISTING CULVERT	FIBER OPTIC
PROPOSED CULVERT (Box or Pipe)	GAS
COMBUSTIBLE FLUIDS	SANITARY SEWER
MARSH AREA	STORM SEWER
WOODED OR SHRUB AREA	TELEPHONE
	WATER
	UTILITY PEDESTAL
	POWER POLE
	TELEPHONE POLE

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION  
PLAN OF PROPOSED IMPROVEMENT  
**C EAU CLAIRE, STATE ST/CTH F**  
HEATHER ROAD TO HAMILTON AVE  
CTH F  
EAU CLAIRE COUNTY

STATE PROJECT NUMBER  
**7995-02-63**



HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS), EAU CLAIRE COUNTY, NAD83 ( 2011 ), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES.

ELEVATIONS ARE REFERENCED TO NAVD 88 ( 2012 ). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 12A

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
7995-02-63	WISC 2024291	1

ACCEPTED FOR  
EAU CLAIRE COUNTY  
HIGHWAY DEPARTMENT  
DATE: 10/6/2023 *Jan Johnson*  
(Highway Commissioner Signature)

ORIGINAL PLANS PREPARED BY  
  
  
DATE: 10-04-2023  
(Professional Engineer Signature)

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION  
PREPARED BY  
Surveyor \_\_\_\_\_ SEH  
Designer \_\_\_\_\_ SEH  
Project Manager \_\_\_\_\_ TYLER RONGSTAD  
Regional Examiner \_\_\_\_\_ TOU YANG  
Regional Supervisor \_\_\_\_\_ TYLER RONGSTAD

APPROVED FOR THE DEPARTMENT  
DATE: \_\_\_\_\_ Tyler Rongstad \_\_\_\_\_  
(Signature)

E

GENERAL NOTES

- WHEN THE QUANTITY OF BASE AGGREGATE OR ASPHALTIC SURFACE IS MEASURED FOR PAYMENT BY THE TON OR CUBIC YARD, THE DEPTH OR THICKNESS OF THE LAYER SHOWN ON THE PLANS IS APPROXIMATE AND THE ACTUAL THICKNESS WILL DEPEND ON THE DISTRIBUTION OF THE MATERIAL AS DIRECTED BY THE ENGINEER.
- THE LOCATION OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS IS APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.
- NO TREES OR SHRUBS ARE TO BE REMOVED WITHOUT THE APPROVAL OF THE ENGINEER AND THE WISDOT REGIONAL ENVIRONMENTAL COORDINATOR IF BETWEEN APRIL 1ST AND OCTOBER 31ST.
- THE EXACT LOCATION OF THE EROSION CONTROL DEVICES SHALL BE DETERMINED IN THE FIELD.
- DISTURBED AREAS WITHIN THE RIGHT-OF-WAY WITHOUT CURB AND GUTTER, EXCEPT THE AREAS WITHIN THE FINISHED SHOULDER POINTS, ARE TO BE 6-INCH SALVAGED TOPSOILED, FERTILIZED, SEEDED AND MULCHED. FINISHED SEEDED SURFACE SHALL BE 1-INCH BELOW THE TOP OF ADJACENT CONCRETE.
- DISTURBED AREAS WITHIN THE RIGHT-OF-WAY WITH CURB AND GUTTER ARE TO BE 6-INCH SALVAGED TOPSOILED, FERTILIZED, SEEDED AND MULCHED. FINISHED SEEDED SURFACE SHALL BE 1-INCH BELOW THE TOP OF ADJACENT CONCRETE.
- THE LOCATION OF ALL DRIVEWAYS WILL BE DETERMINED BY THE ENGINEER.
- ALL CURB AND GUTTER RADII, PAVEMENT DIMENSIONS AND STATIONS ARE SHOWN TO THE EDGE OF PAVEMENT UNLESS NOTED OTHERWISE.
- ALL SIDE ROAD EARTHWORK QUANTITIES ARE INCLUDED IN MAINLINE EARTHWORK QUANTITIES.
- CONSTRUCT INSIDE EDGE OF SIDEWALK ¼ INCH HIGHER THAN THE TOP OF CURB, WHEN THEY ARE ADJACENT TO EACH OTHER.
- A VERTICAL SAWCUT SHALL BE MADE THROUGH EXISTING DRIVEWAYS AND PAVEMENTS AT REMOVAL LIMITS.
- TOP OF CASTING ELEVATIONS SHOWN FOR INLETS REFER TO THE CASTING ELEVATION AT THE FRONT EDGE OF CASTING/FLOWLINE OF GRATE/TOP OF CURB BOX.
- ALL STORM SEWER INVERTS, ELEVATIONS, PIPE LENGTHS, AND GRADES ARE COMPUTED CENTER-TO-CENTER OF STRUCTURES.
- CURB RAMP TYPES ARE SHOWN ON THE CURB RAMP DETAIL SHEETS.
- TYPICAL PROPOSED DRIVEWAY WIDTH IS 18 FEET, AS MEASURED AT THE BACK OF PATH OR SIDEWALK, UNLESS SHOWN DIFFERENTLY ON THE PLAN.
- SIDEROAD PAVEMENT STRUCTURE SHALL BE THE SAME AS THE MAINLINE.

**RUNOFF COEFFICIENT TABLE**

	HYDROLOGIC SOIL GROUP											
	A			B			C			D		
	SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)		
LAND USE:	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER
ROW CROPS	.08	.16	.22	.12	.20	.27	.15	.24	.33	.19	.28	.38
	.22	.30	.38	.26	.34	.44	.30	.37	.50	.34	.41	.56
MEDIAN STRIP-TURF	.19	.20	.24	.19	.22	.26	.20	.23	.30	.20	.25	.30
	.24	.26	.30	.25	.28	.33	.26	.30	.37	.27	.32	.40
SIDE SLOPE-TURF			.25			.27			.28			.30
			.32			.34			.36			.38
PAVEMENT:												
ASPHALT	.70 - .95											
CONCRETE	.80 - .95											
BRICK	.70 - .80											
DRIVES, WALKS	.75 - .85											
ROOFS	.75 - .95											
GRAVEL ROADS, SHOULDERS	.40 - .60											

TOTAL PROJECT AREA = 7.5 ACRES  
 TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = 5.9 ACRES

UTILITY CONTACTS

- AT&T - COMMUNICATION LINE  
 304 S DEWEY STREET  
 EAU CLAIRE, WI 54701  
 TELEPHONE:715.410.0656 (MOBILE)  
 ATTENTION:RICK PODOLAK  
 EMAIL:RP4514@ATT.COM
- CHARTER COMMUNICATIONS - COMMUNICATION LINE  
 1201 MCCANN DRIVE  
 ALTOONA, WI 54720  
 TELEPHONE:715.833.7390, 715.215.0842 (MOBILE)  
 ATTENTION:RYAN LUCKEN  
 EMAIL:RYAN.LUCKEN@CHARTER.COM
- CITY OF EAU CLAIRE - WATER  
 1040 FOREST STREET  
 EAU CLAIRE, WI 54703  
 TELEPHONE:715.839.5189  
 ATTENTION:MIKE MANCL  
 EMAIL:MIKE.MANCL@EAUCLAIREWI.GOV
- CITY OF EAU CLAIRE - SANITARY SEWER  
 1040 FOREST STREET  
 EAU CLAIRE, WI 54703  
 TELEPHONE:715.839.4950  
 ATTENTION:JAHN EBERHARDT  
 EMAIL:JAHN.EBERHARDT@EAUCLAIREWI.GOV
- XCEL ENERGY - ELECTRICITY  
 1400 WESTERN AVE  
 EAU CLAIRE, WI 54701  
 TELEPHONE:715.737.6035  
 ATTENTION:JEFF GALLAGHER  
 EMAIL:JEFF.J.GALLAGHER@XCELENERGY.COM
- XCEL ENERGY - GAS / PETROLEUM  
 1400 WESTERN AVE  
 EAU CLAIRE, WI 54701  
 TELEPHONE:715-928-3459  
 ATTENTION:BENJAMEN CARLI  
 EMAIL:BENJAMEN.J.CARLI@XCELENERGY.COM

DESIGN CONTACT  
 10 NORTH BRIDGE STREET  
 CHIPPEWA FALLS, WI 54729  
 TELEPHONE:715.720.6291  
 ATTENTION:JUSTIN SHAVLIK  
 EMAIL:JSHAVLIK@SEHINC.COM

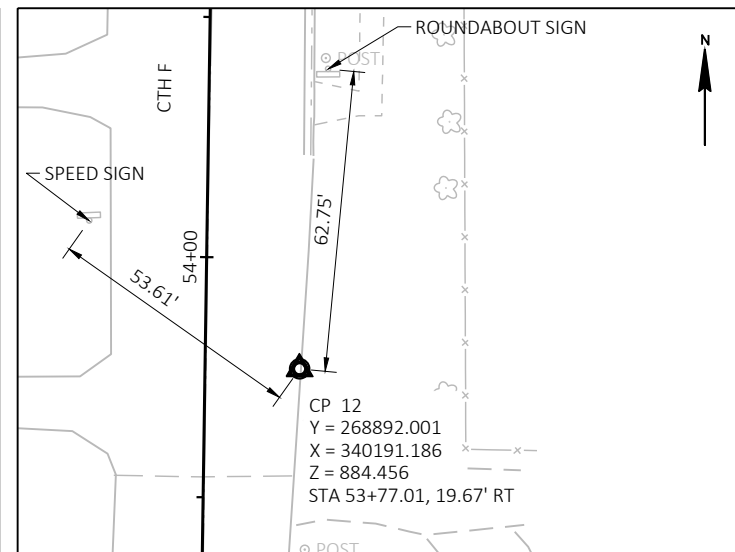
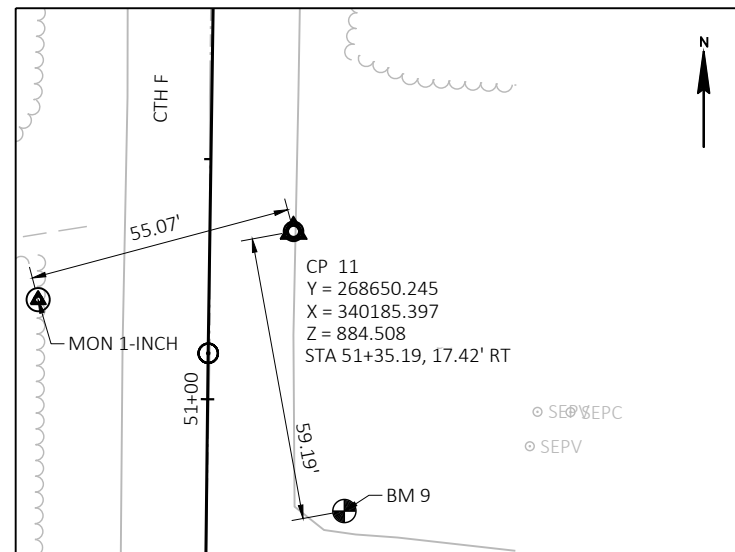
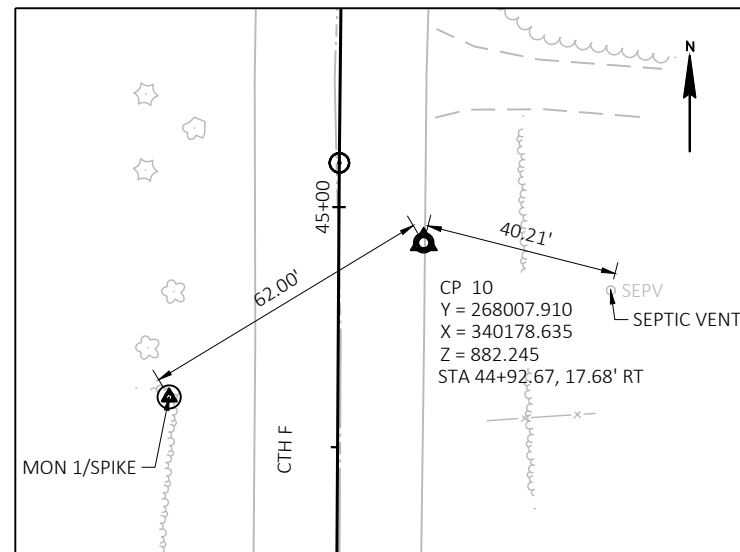
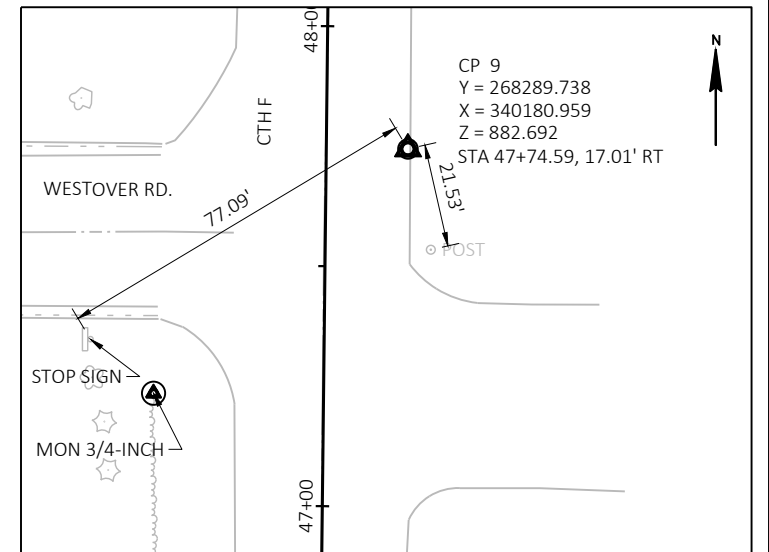
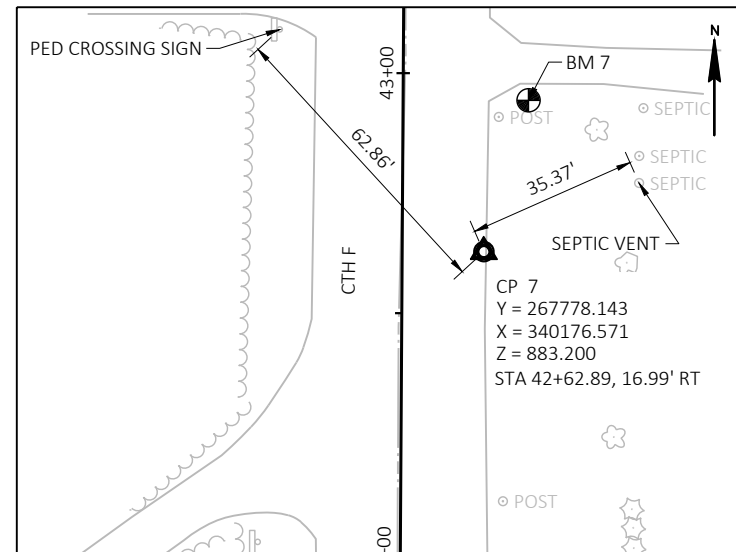
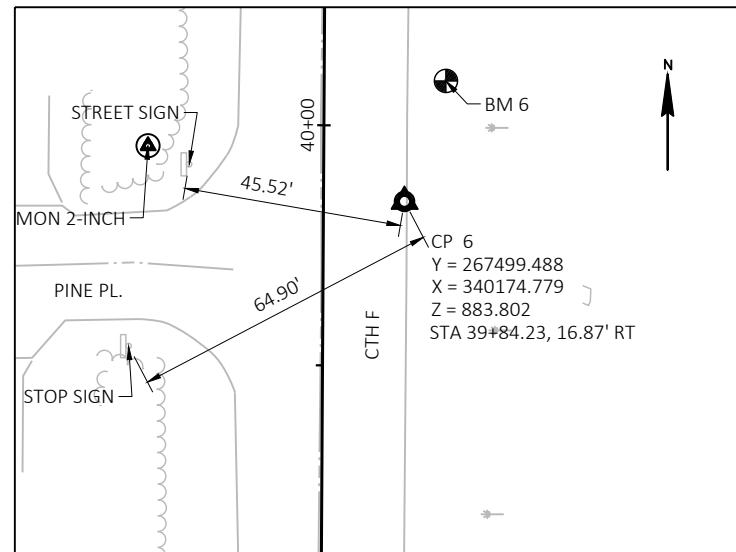
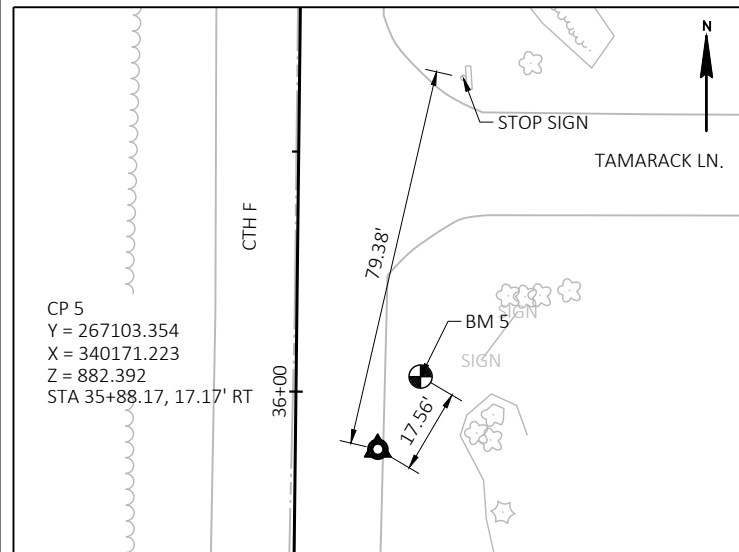
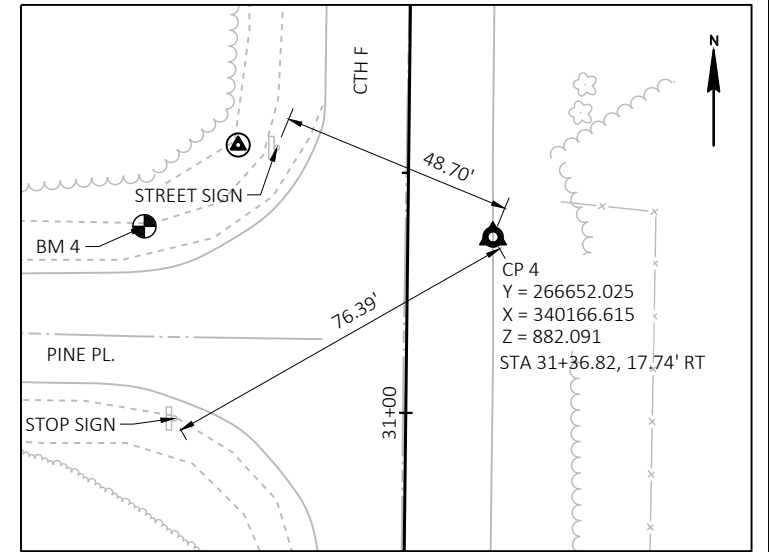
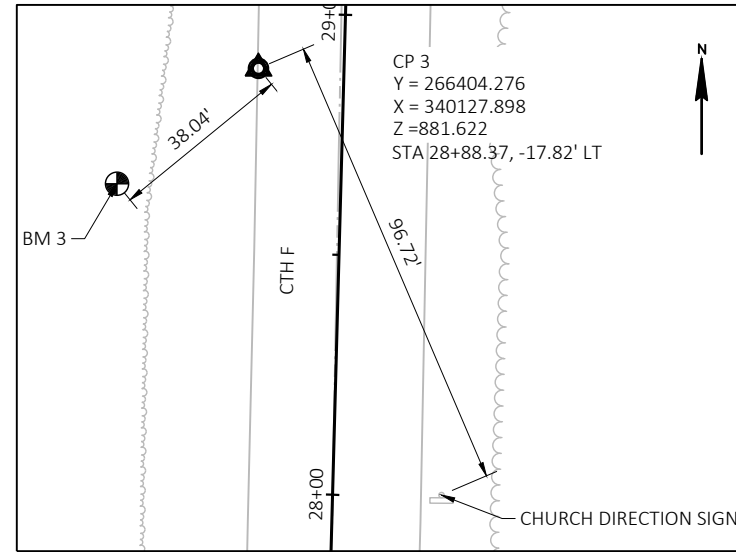
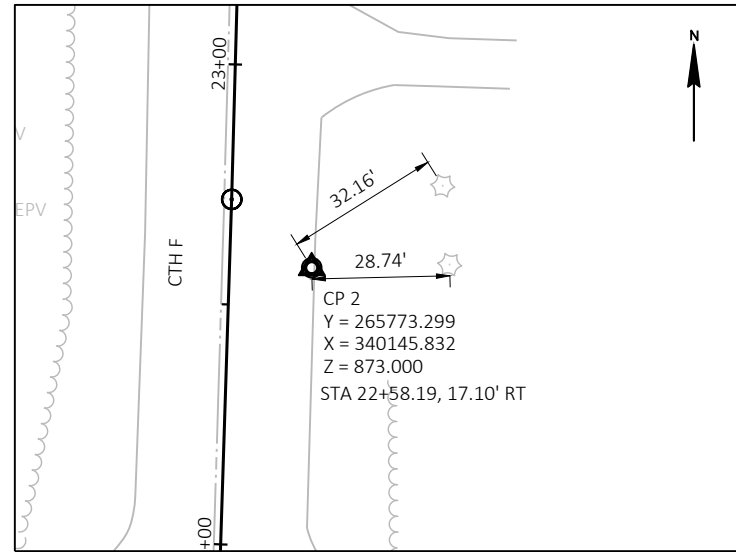
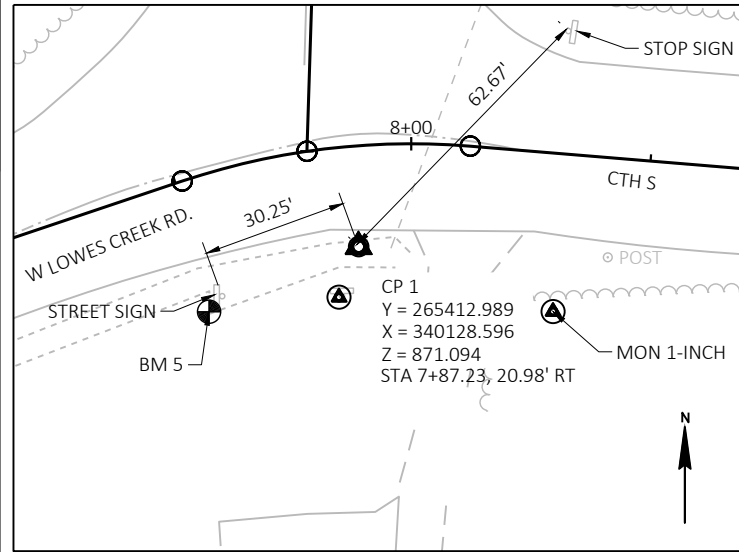
OWNER CONTACT  
 EAU CLAIRE COUNTY HIGHWAY COMMISSIONER  
 5061 US HWY 53  
 EAU CLAIRE, WI 54701  
 TELEPHONE:715.839.1042  
 ATTENTION:JON JOHNSON  
 EMAIL:JON.JOHNSON@EAUCLAIRECOUNTY.GOV

WISDOT CONTACT  
 NORTHWEST REGION HQ  
 718 WEST CLAIREMONT AVENUE  
 EAU CLAIRE, WI 54701  
 TELEPHONE:715.461.0372  
 ATTENTION:TYLER RONGSTAD  
 EMAIL:TYLER.RONGSTAD@DOT.WI.GOV

WDNR CONTACT  
 DNR WEST CENTRAL REGION HQ  
 1300 WEST CLAIREMONT AVENUE  
 EAU CLAIRE, WI 54701  
 TELEPHONE:715.934.9014  
 ATTENTION:LEAH.NICOL  
 EMAIL:LEAH.NICOL@WISCONSIN.GOV



Dial **811** or (800)242-8511  
 www.DiggersHotline.com

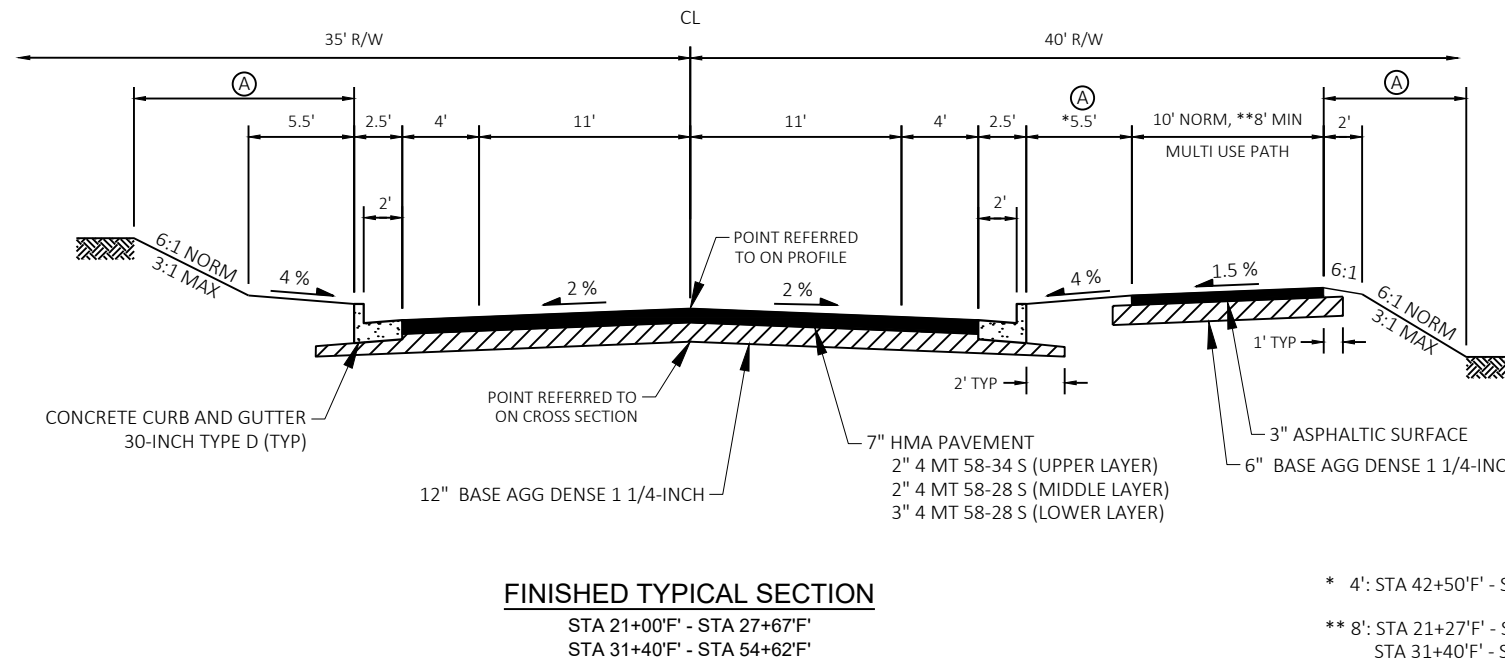
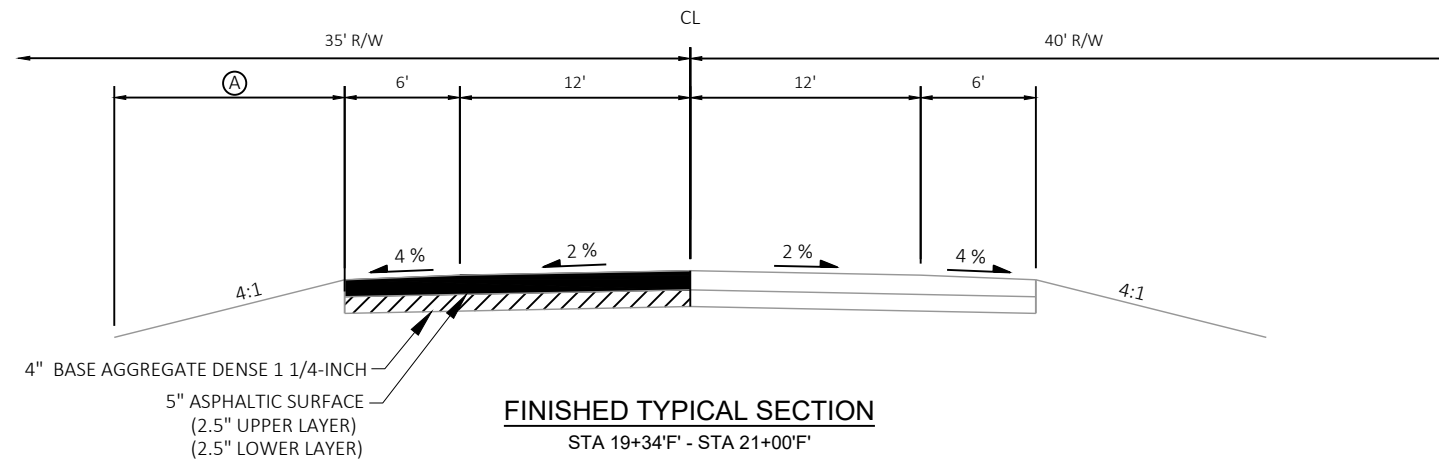
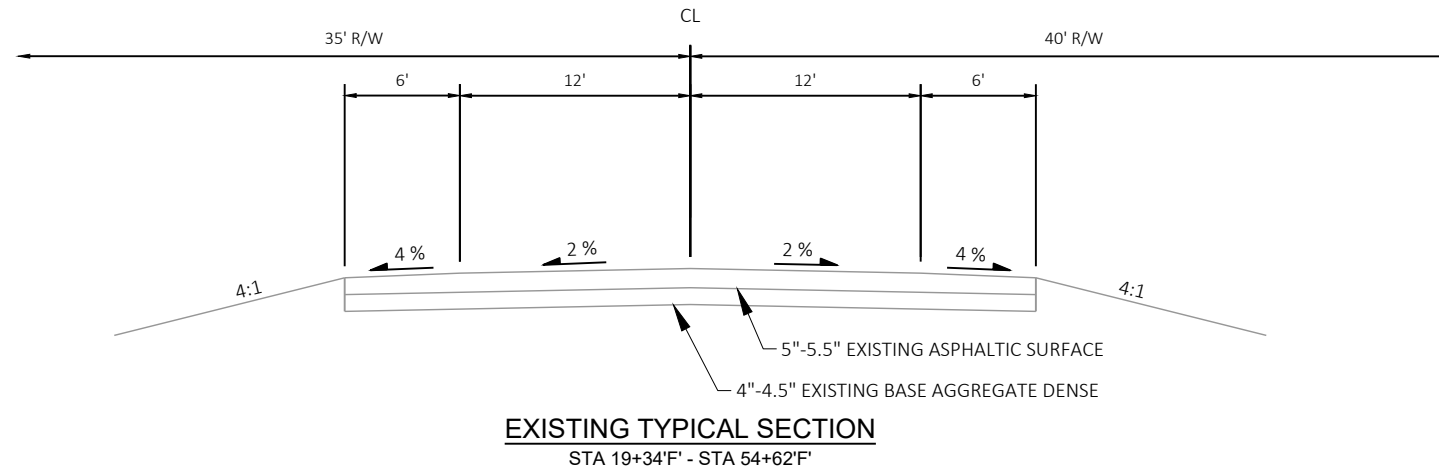




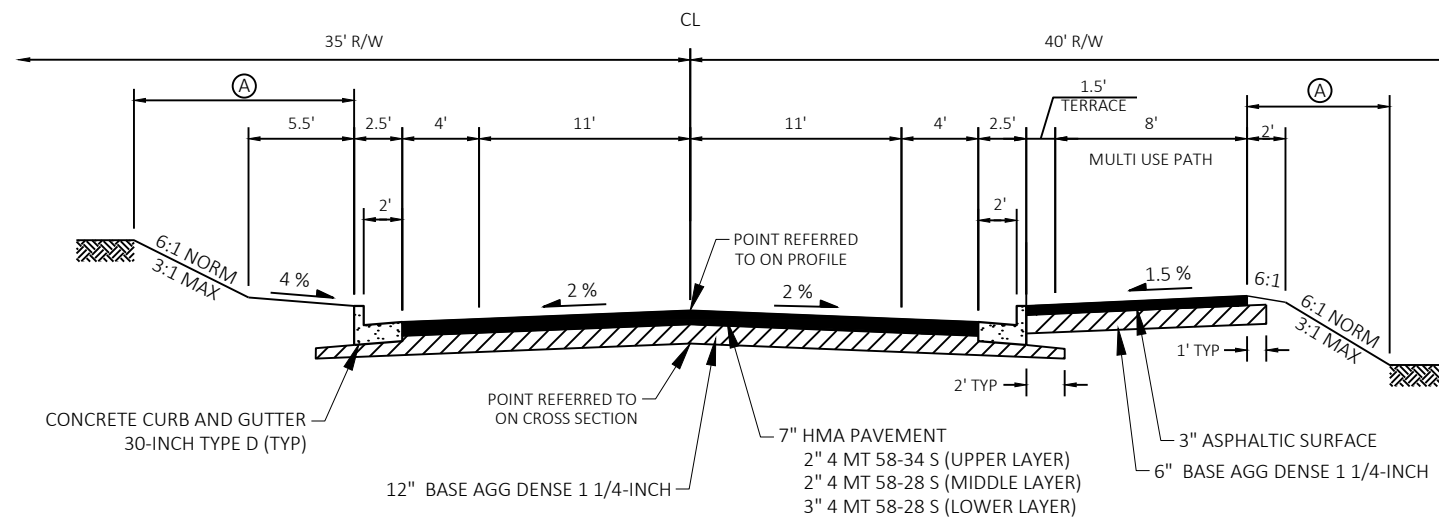
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PROJECT NO: 7995-02-63	HWY: CTH F	COUNTY: EAU CLAIRE	PROJECT OVERVIEW	SHEET	E
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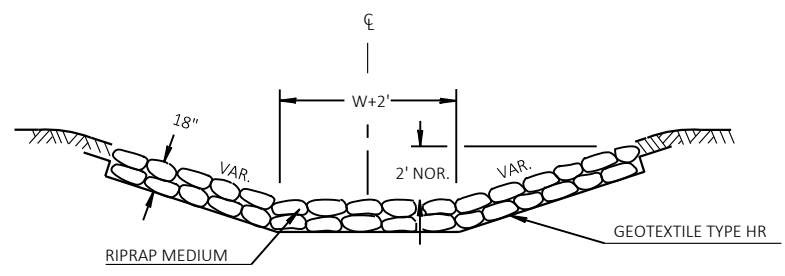
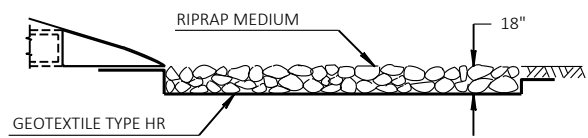
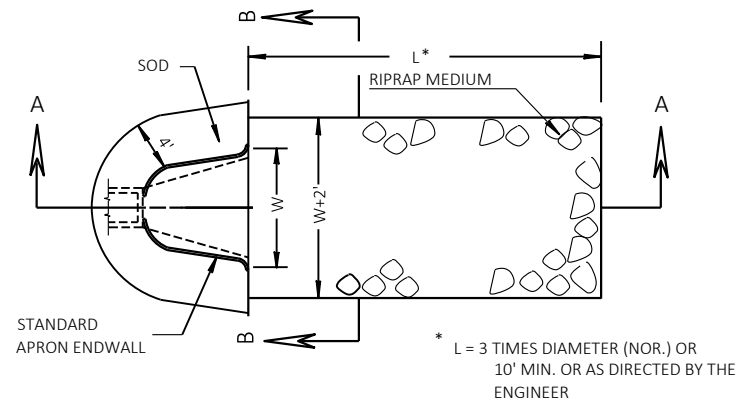


Ⓐ 6" SALVAGED TOPSOIL, SEEDING MIXTURE NO. 40, TEMPORARY SEED, FERTILIZER TYPE B, AND MULCHING (MULCHING NOT REQ'D WHERE EMAT IS SHOWN ON PLAN SHEETS)

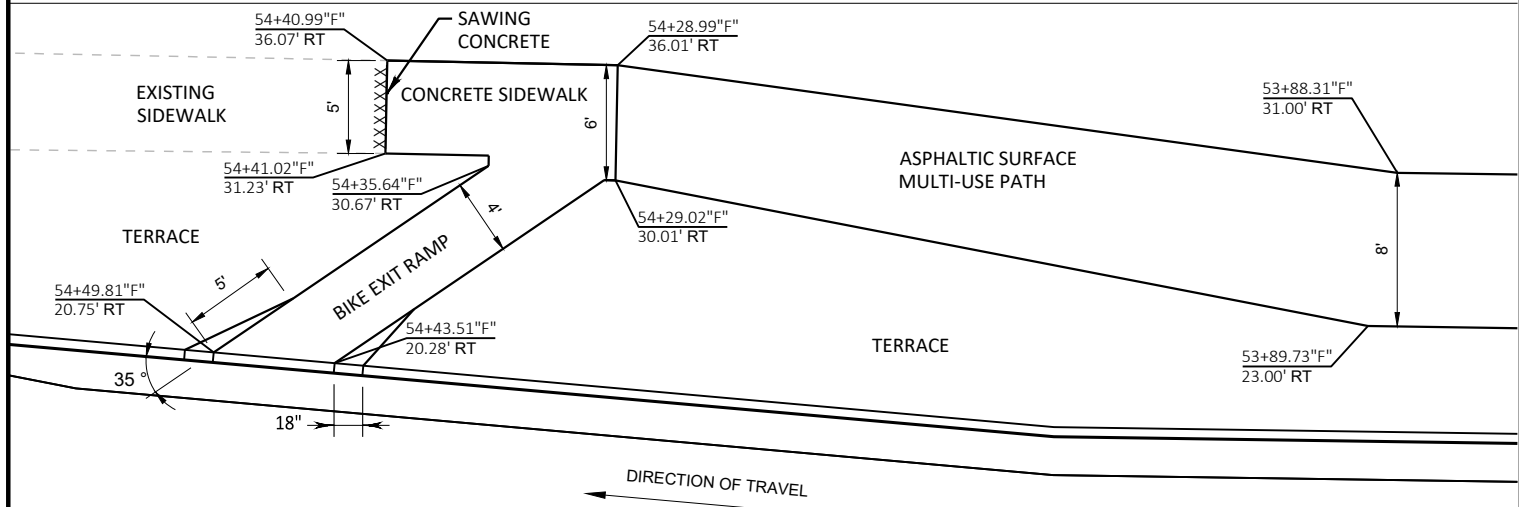


Ⓐ 6" SALVAGED TOPSOIL, SEEDING MIXTURE NO. 40, TEMPORARY SEED, FERTILIZER TYPE B, AND MULCHING (MULCHING NOT REQ'D WHERE EMAT IS SHOWN ON PLAN SHEETS)

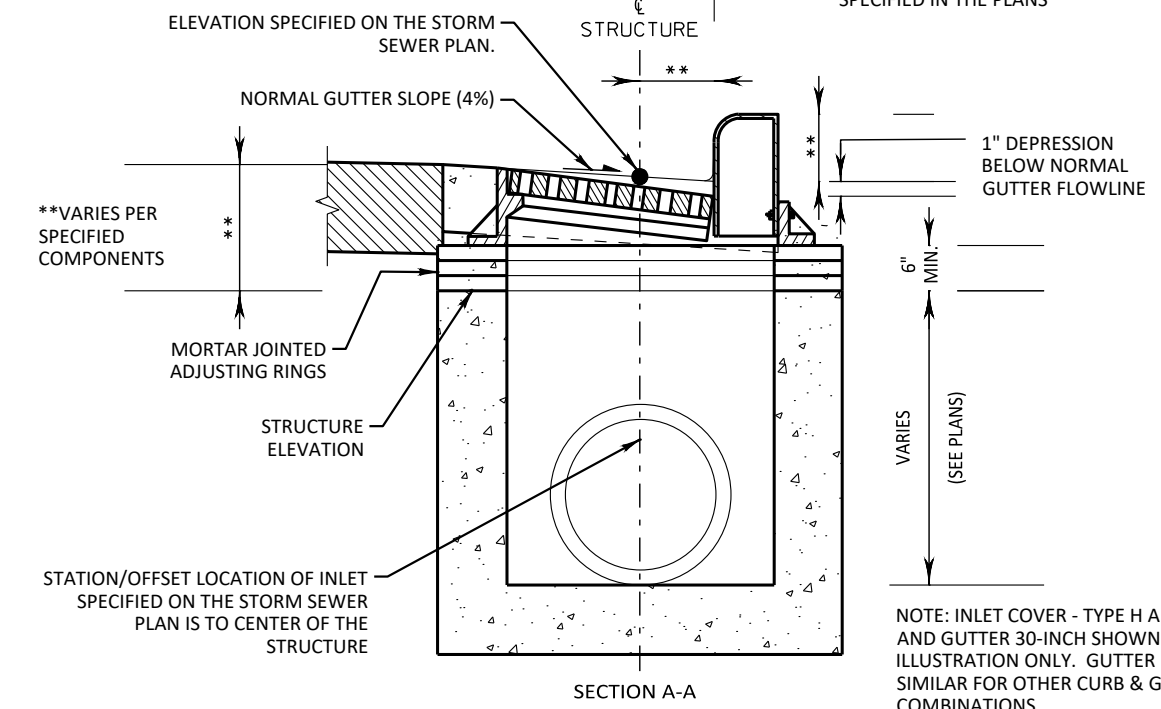
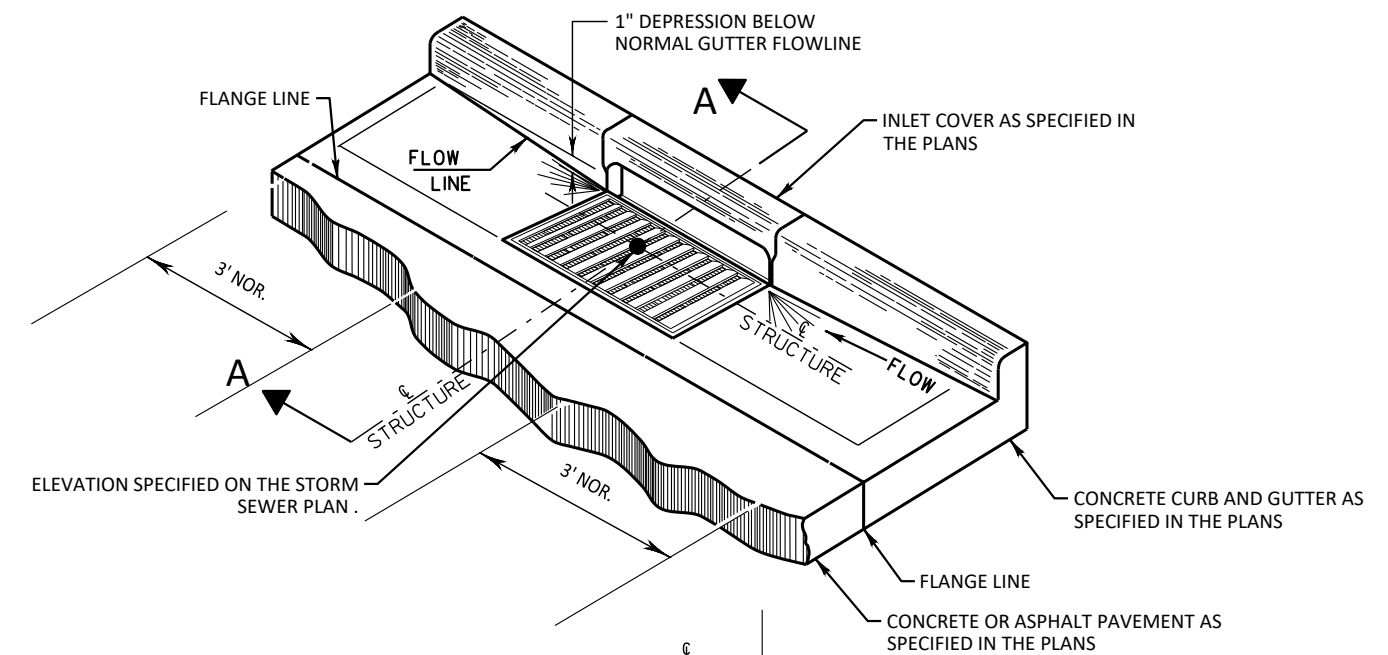
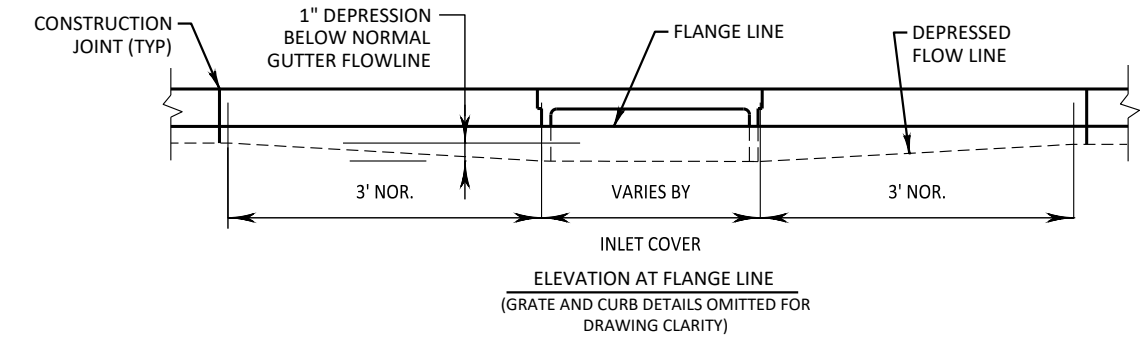
**FINISHED TYPICAL SECTION**  
STA 27+67'F' - STA 31+40'F'



MEDIUM RANDOM RIPRAP AND GEOTEXTILE FABRIC DETAIL AT APRON ENDWALL  
STA 7+00 'LC'

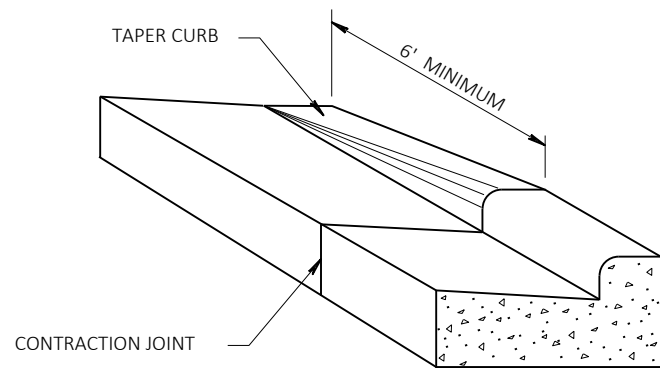


BIKE EXIT RAMP  
STA 54+46' F'  
TO BE PAID FOR AS CONCRETE SIDEWALK 5-INCH



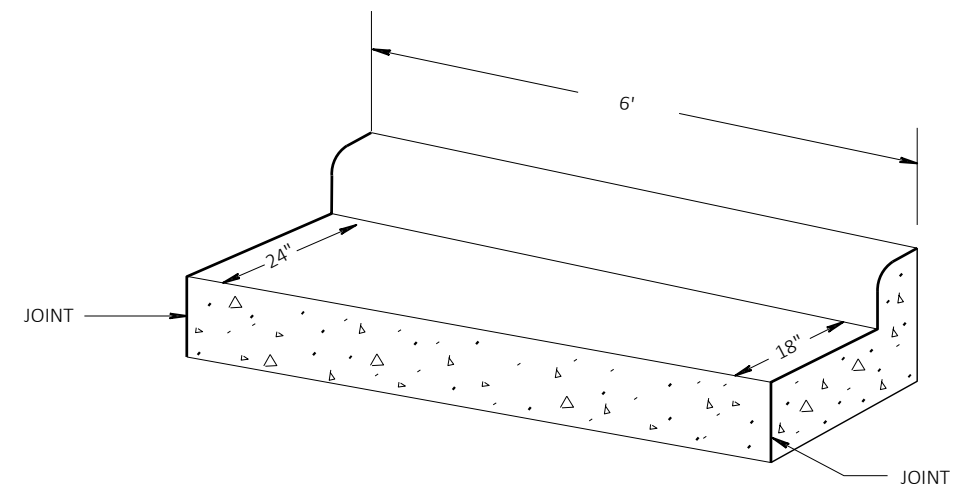
TYPICAL GUTTER PAN DEPRESSION AT INLETS

NOTE: INLET COVER - TYPE H AND CONCRETE CURB AND GUTTER 30-INCH SHOWN FOR DETAIL ILLUSTRATION ONLY. GUTTER PAN DEPRESSION IS SIMILAR FOR OTHER CURB & GUTTER/INLET COVER COMBINATIONS.



DETAIL OF CURB & GUTTER TERMINI

STA 21+00 LT  
 STA 21+09 RT  
 SIDEROADS

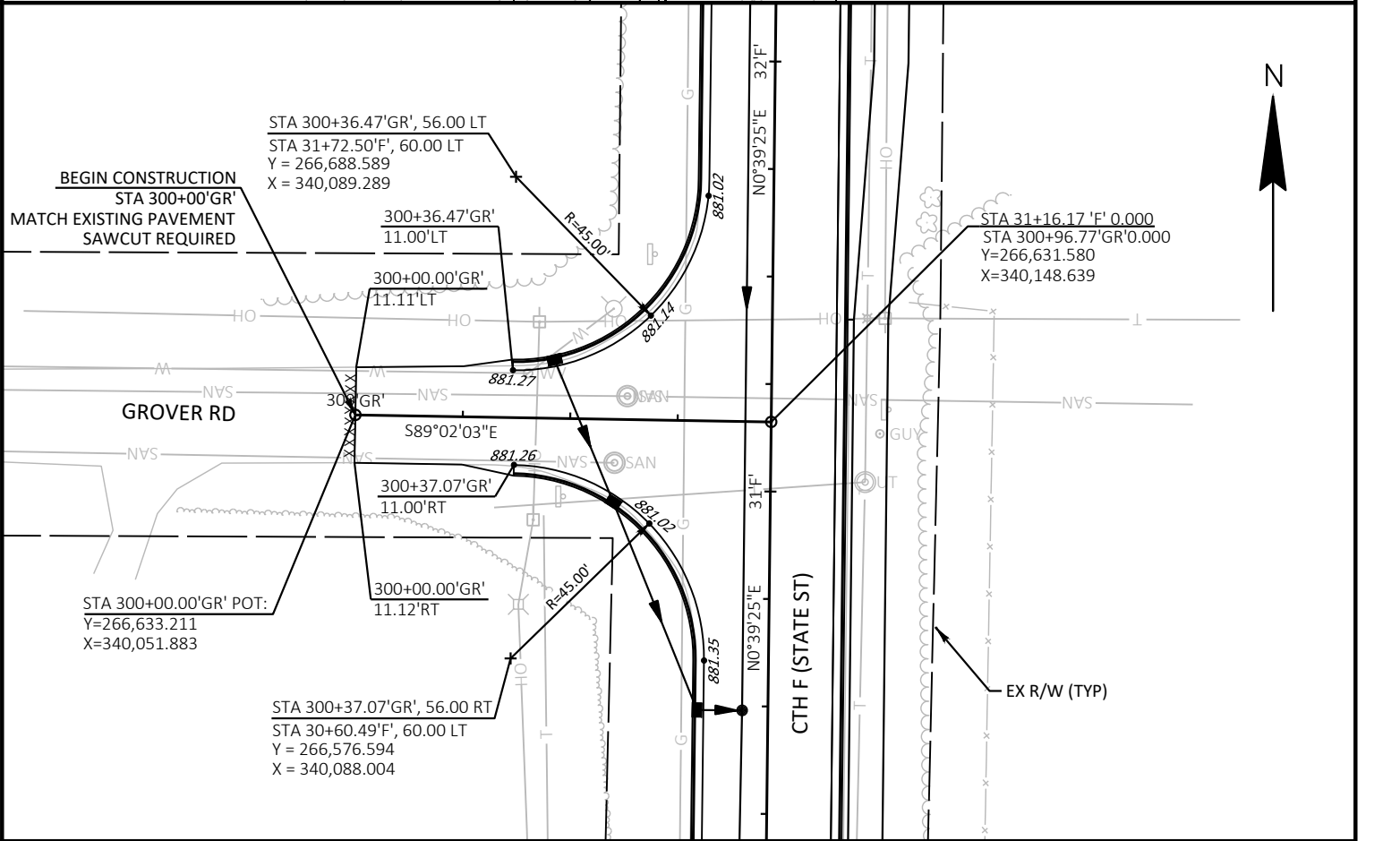
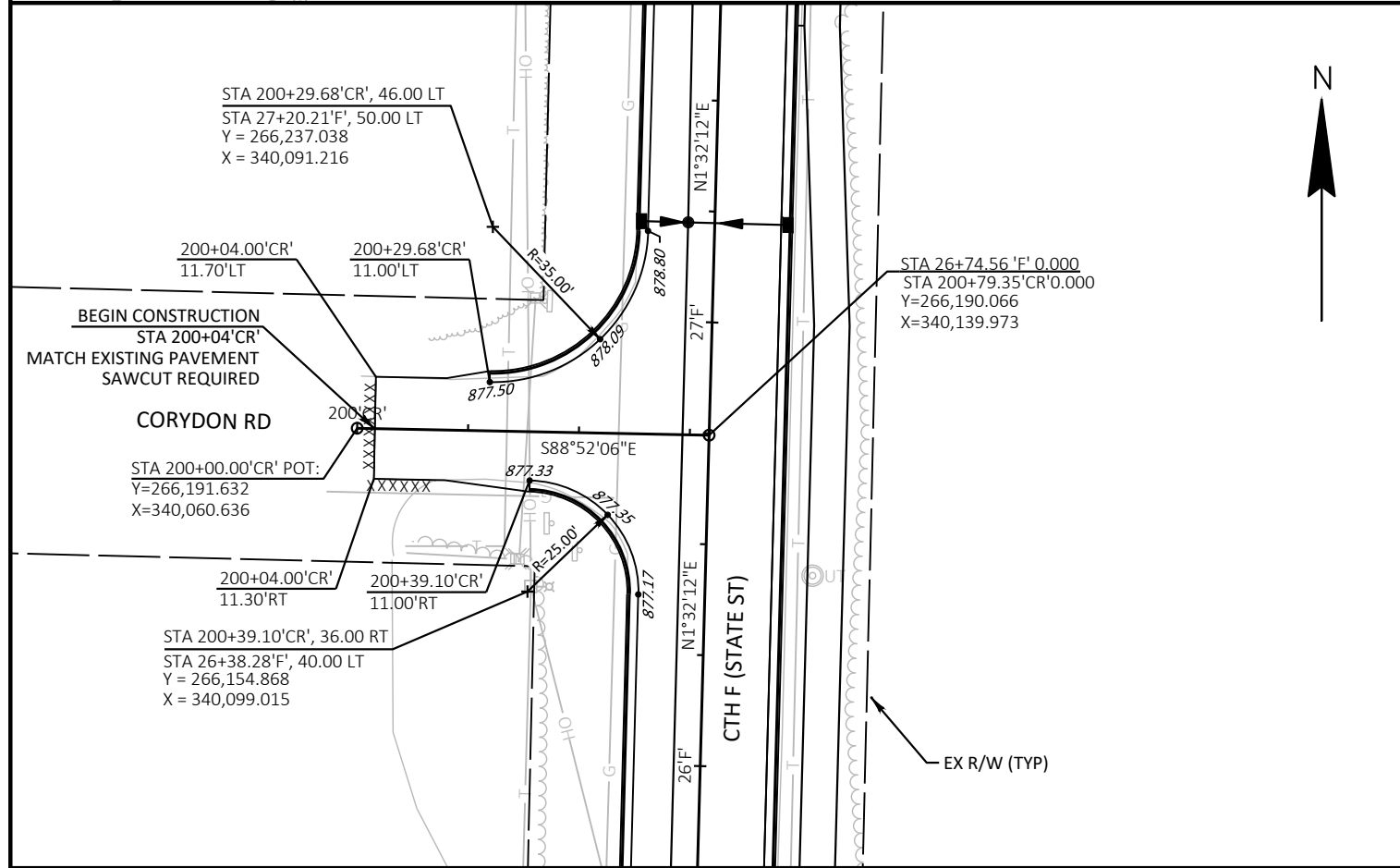
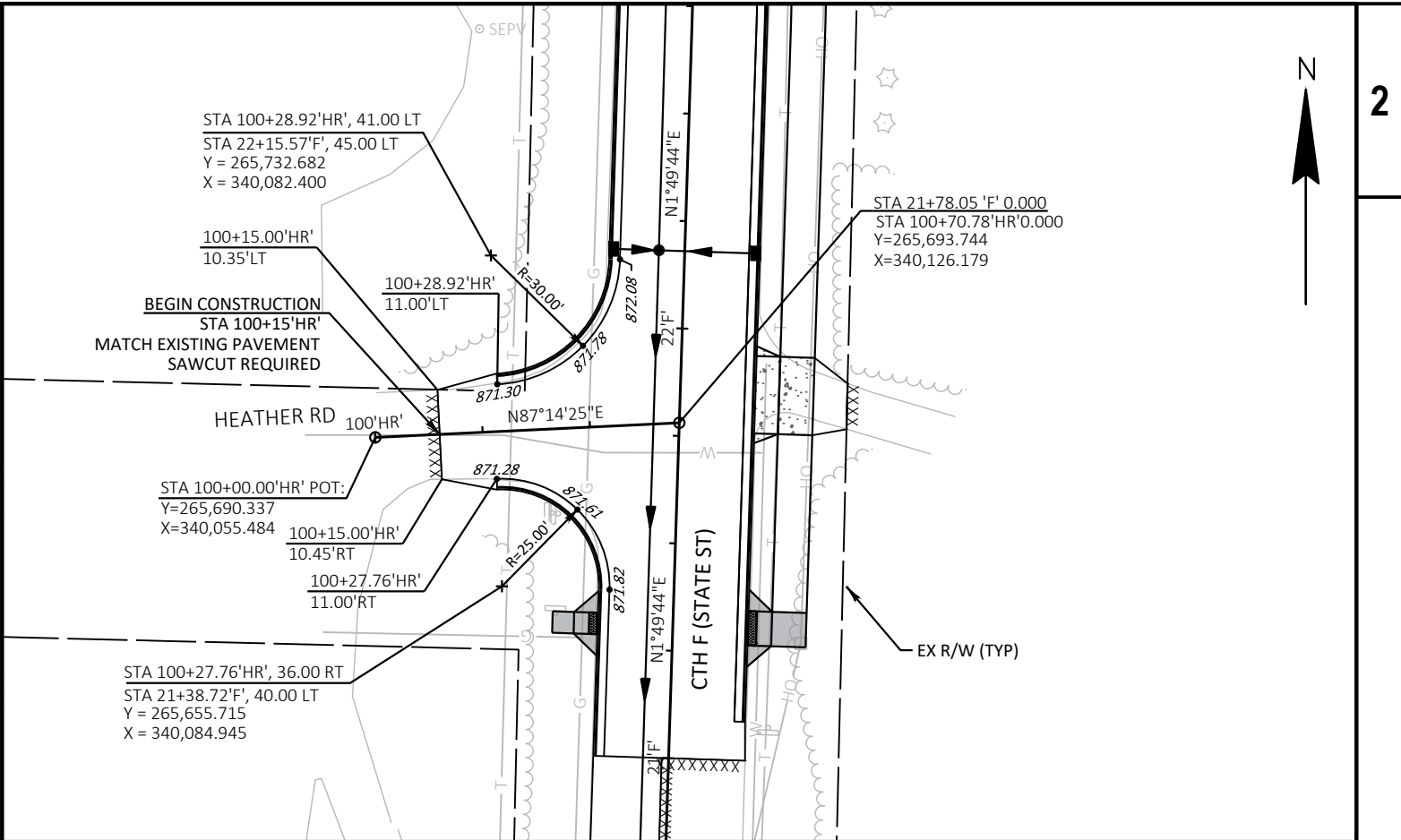
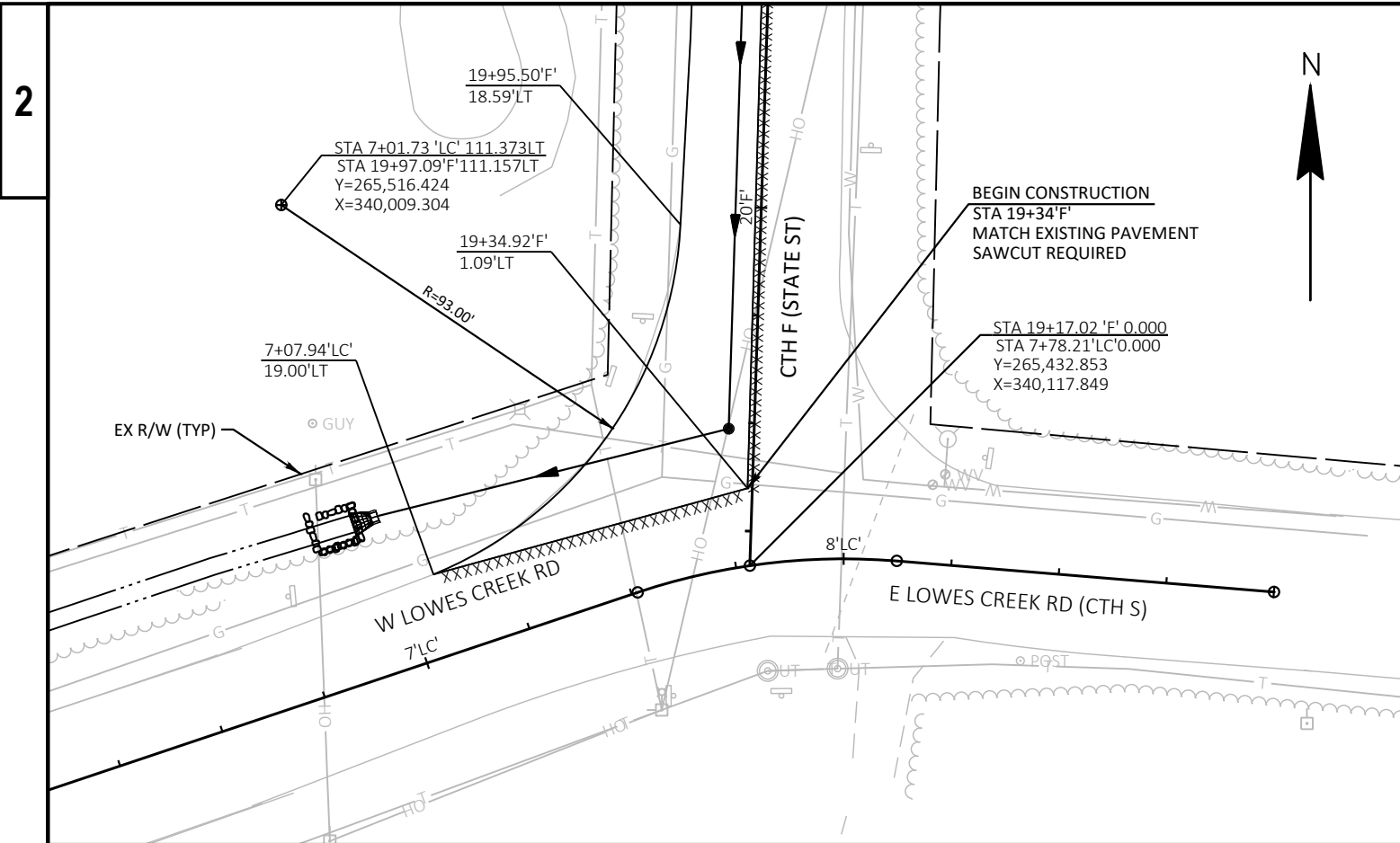


TRANSITION DETAIL

30" TYPE "D" CURB & GUTTER TO 24" TYPE "D" CURB & GUTTER  
 (TO BE MEASURED & PAID FOR AS 30" CONC. C&G)

STA 54+62 LT&RT





PROJECT NO: 7995-02-63

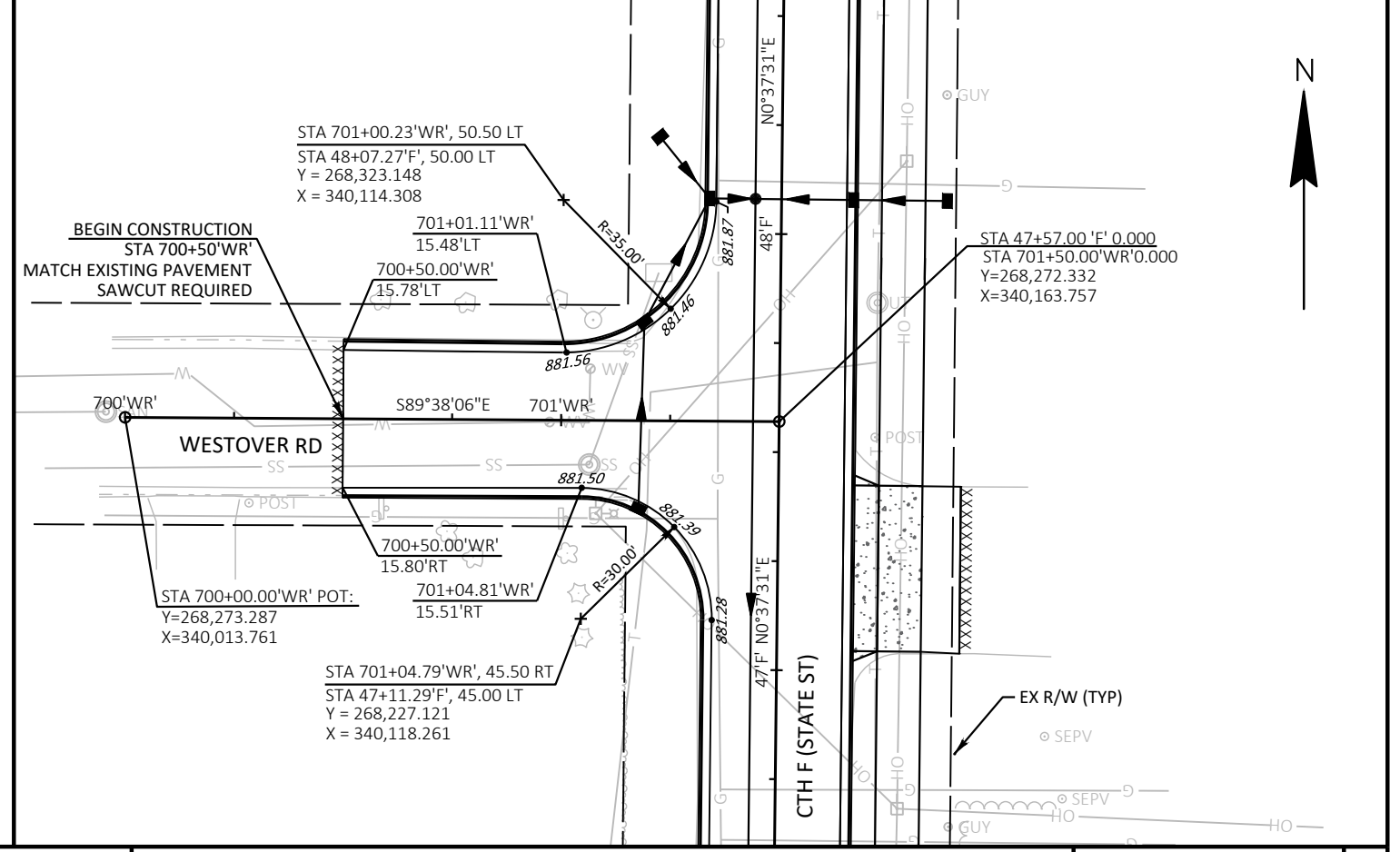
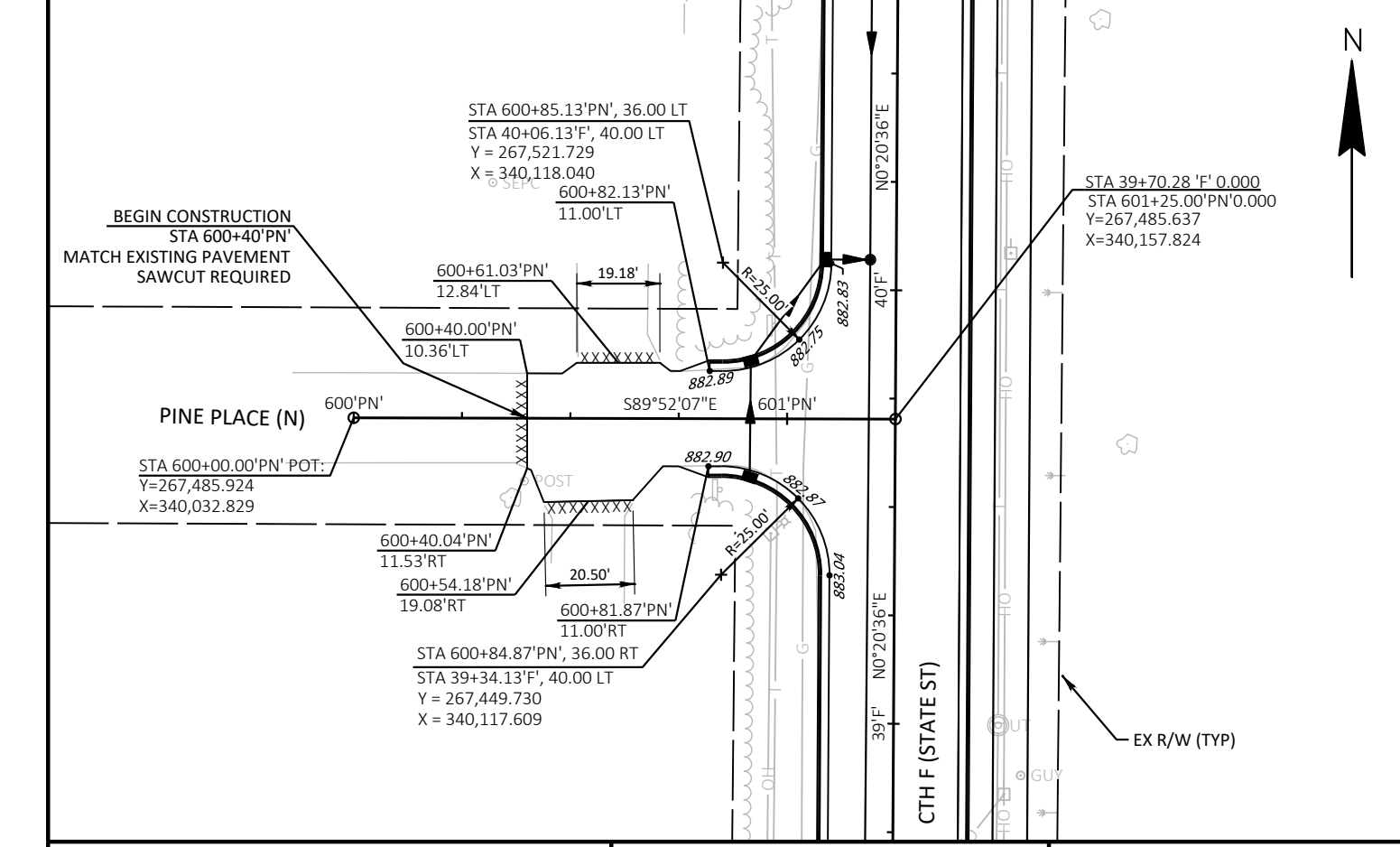
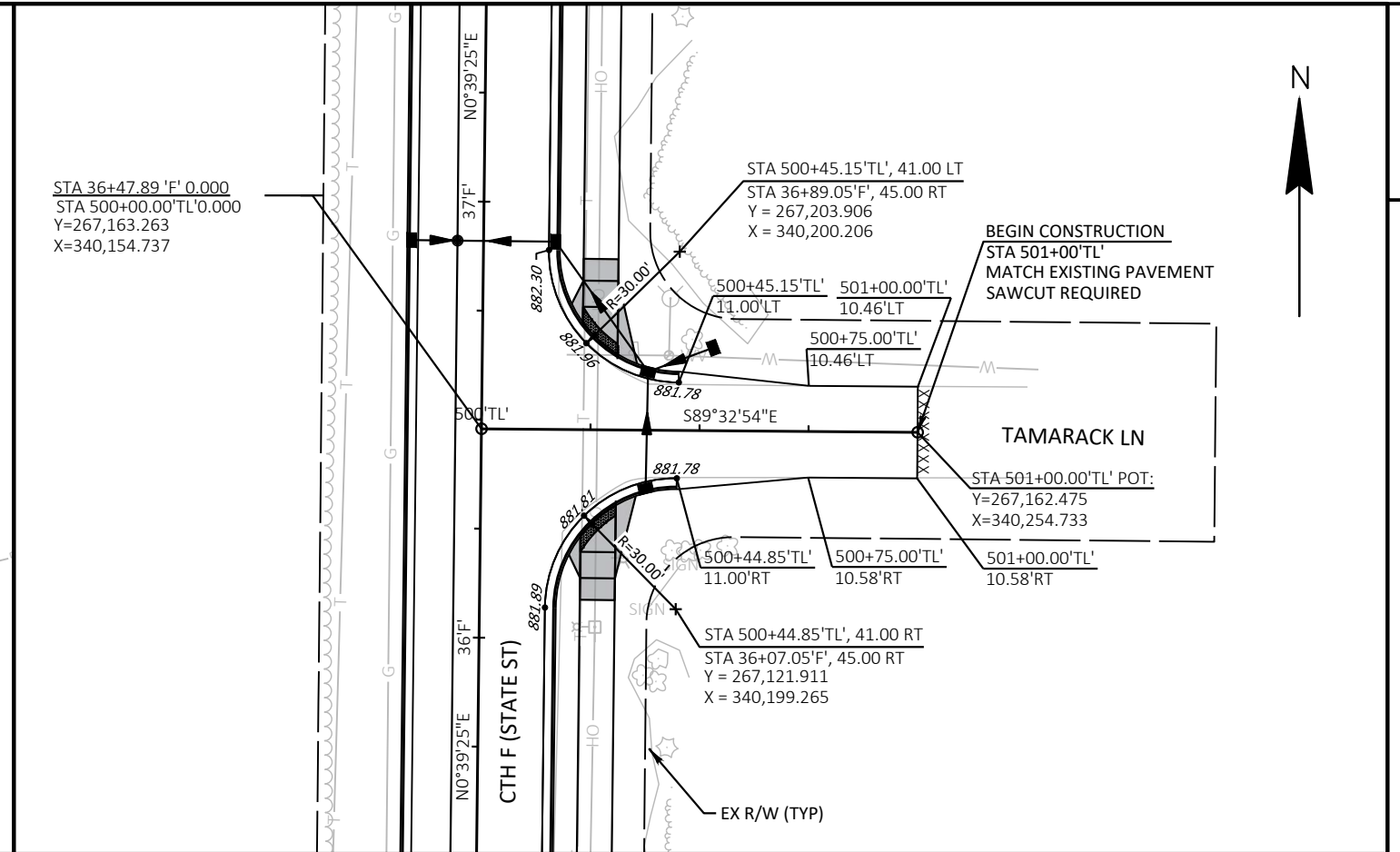
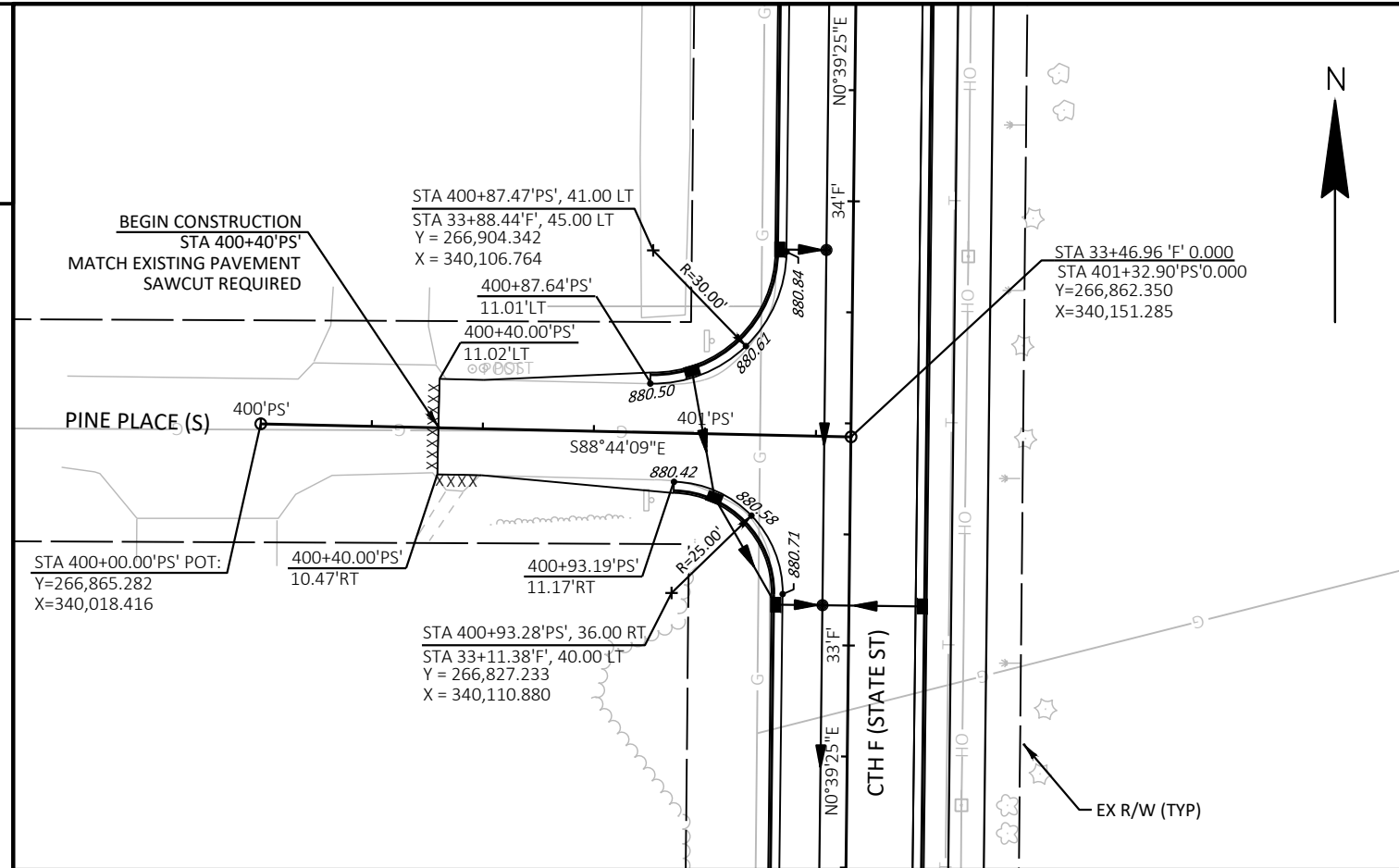
HWY: CTH F

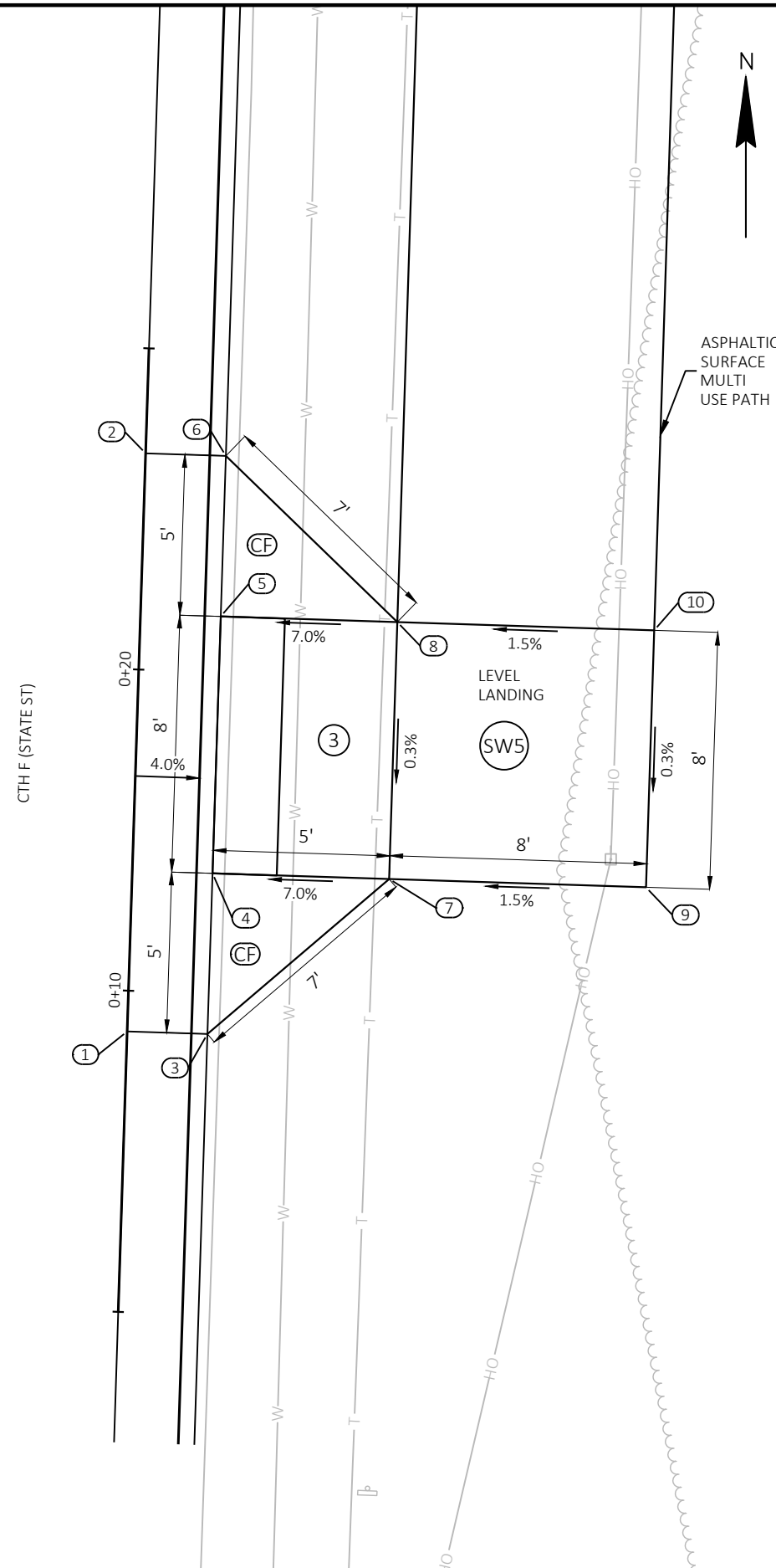
COUNTY: EAU CLAIRE

INTERSECTION DETAILS

SHEET

E



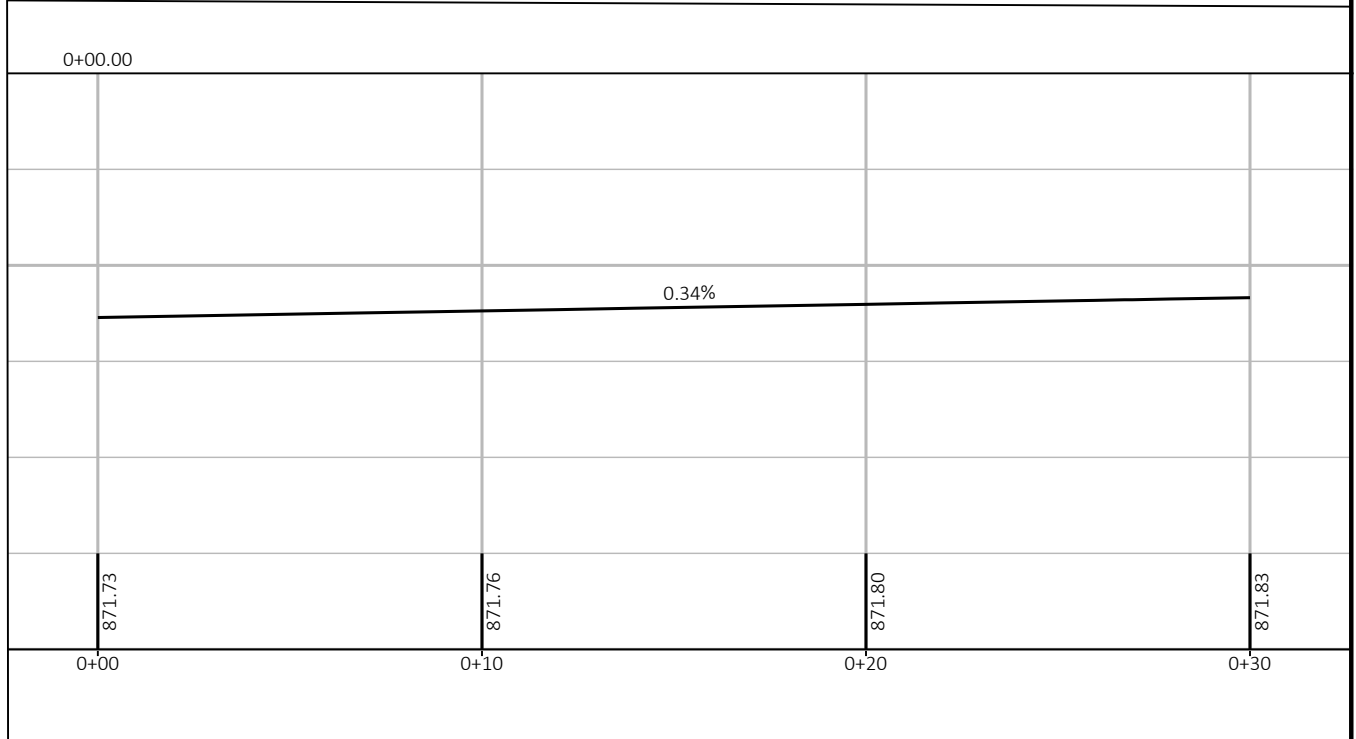
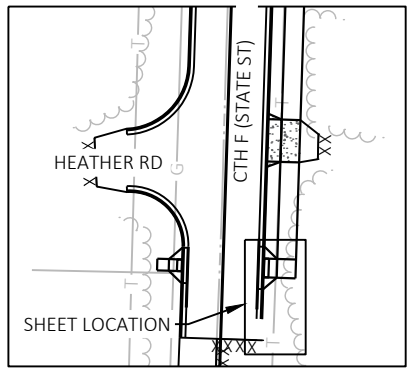


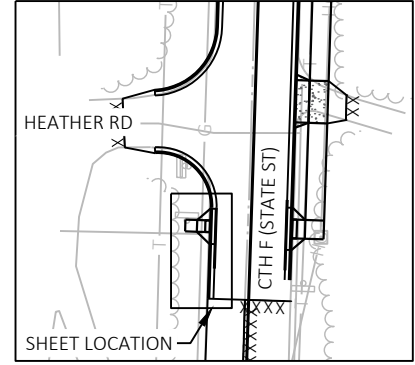
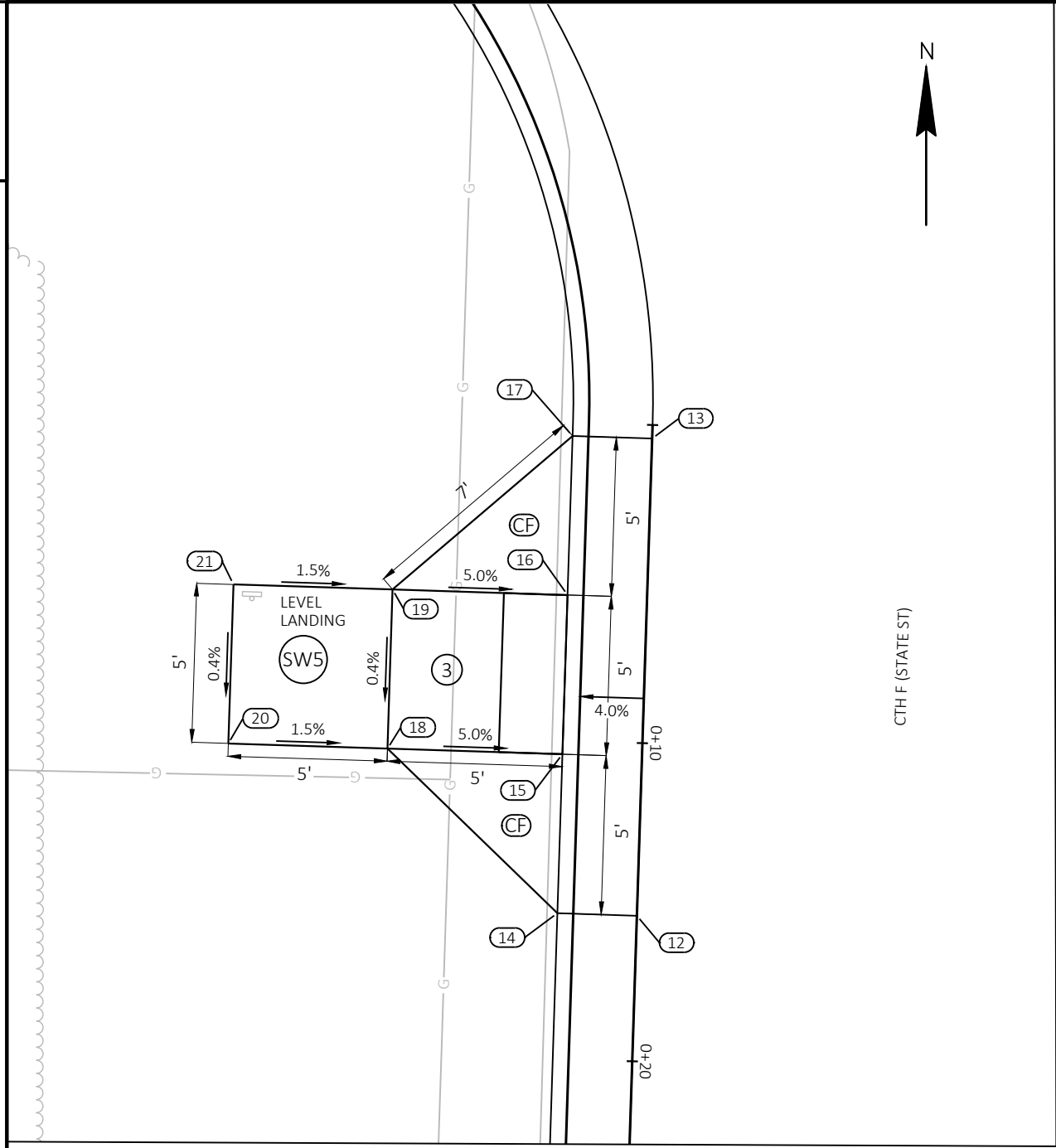
**LEGEND**

- (X) CURB RAMP TYPE
- (SW5) CONCRETE SIDEWALK 5-INCH
- (XX) POINT NUMBER
- (CF) CONCRETE FLARE

HEATHER RD SE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1	21+21.79	15.00' RT	871.76	265637.04	340139.38
2	21+39.79	15.00' RT	871.82	265655.03	340139.95
3	21+21.79	17.50' RT	872.18	265636.96	340141.88
4	21+26.79	17.50' RT	871.70	265641.96	340142.03
5	21+34.79	17.50' RT	871.72	265649.96	340142.29
6	21+39.79	17.50' RT	872.24	265654.95	340142.45
7	21+26.79	23.00' RT	872.08	265641.78	340147.53
8	21+34.79	23.00' RT	872.11	265649.78	340147.79
9	21+26.79	31.00' RT	872.20	265641.53	340155.53
10	21+34.79	31.00' RT	872.23	265649.52	340155.78

- NOTES:**
- CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES, LENGTHS AND MATCH POINTS PRIOR TO CURB RAMP AND SIDEWALK CONSTRUCTION.
  - THE ENGINEER MAY ADJUST ELEVATIONS TO FIT FIELD CONSTRUCTION.
  - SEE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS.
  - ALL STATION OFFSET INFORMATION REFERENCE CTH F (STATE ST) ALIGNMENT.

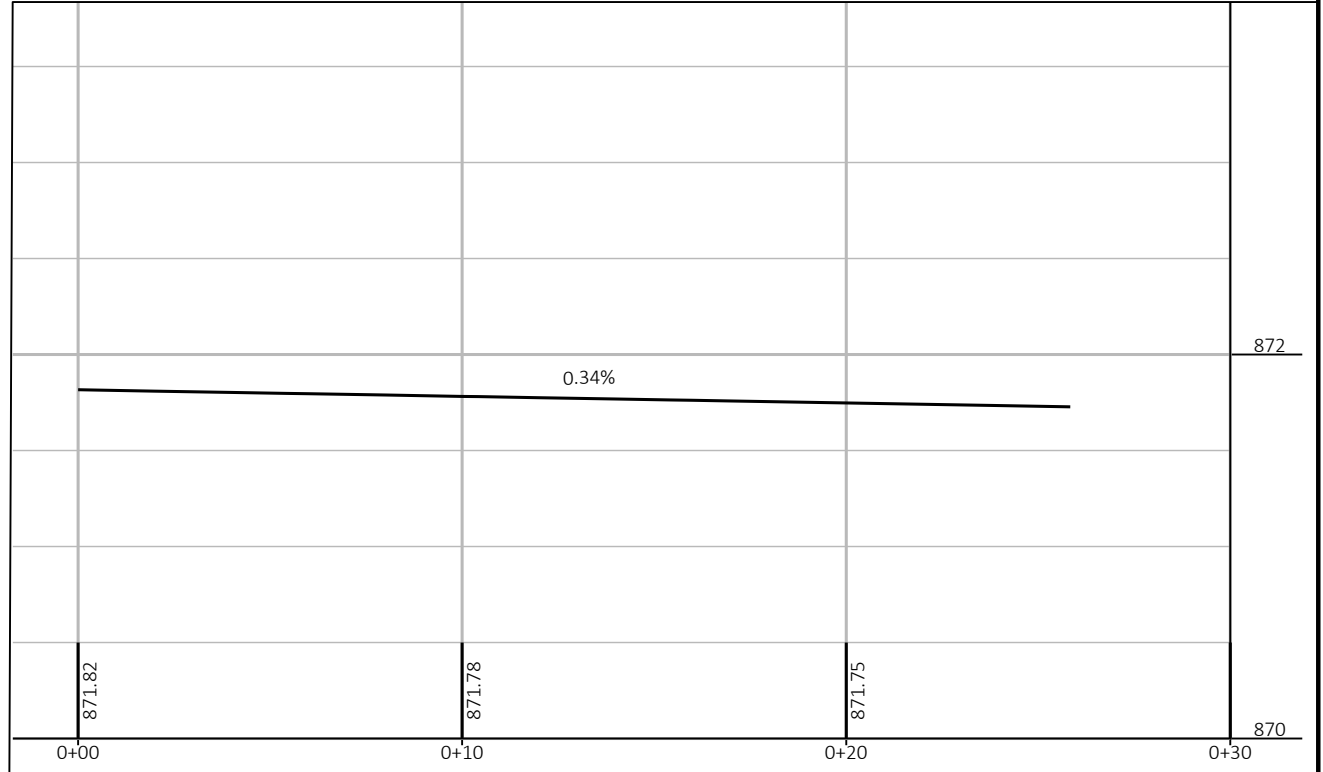




HEATHER RD SW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
12	21+23.29	15.00' LT	871.76	265639.50	340109.44
13	21+38.29	15.00' LT	871.82	265654.49	340109.92
14	21+23.29	17.50' LT	872.18	265639.58	340106.94
15	21+28.29	17.50' LT	872.20	265644.58	340107.10
16	21+33.30	17.50' LT	871.72	265649.58	340107.26
17	21+38.29	17.50' LT	872.24	265654.57	340107.42
18	21+28.29	23.00' LT	872.42	265644.75	340101.60
19	21+33.29	23.00' LT	871.99	265649.75	340101.76
20	21+28.29	28.00' LT	872.05	265644.91	340096.61
21	21+33.29	28.00' LT	872.07	265649.91	340096.77

NOTES:  
 1. CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES, LENGTHS AND MATCH POINTS PRIOR TO CURB RAMP AND SIDEWALK CONSTRUCTION.  
 2. THE ENGINEER MAY ADJUST ELEVATIONS TO FIT FIELD CONSTRUCTION.  
 3. SEE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS.  
 4. ALL STATION OFFSET INFORMATION REFERENCE CTH F (STATE ST) ALIGNMENT.

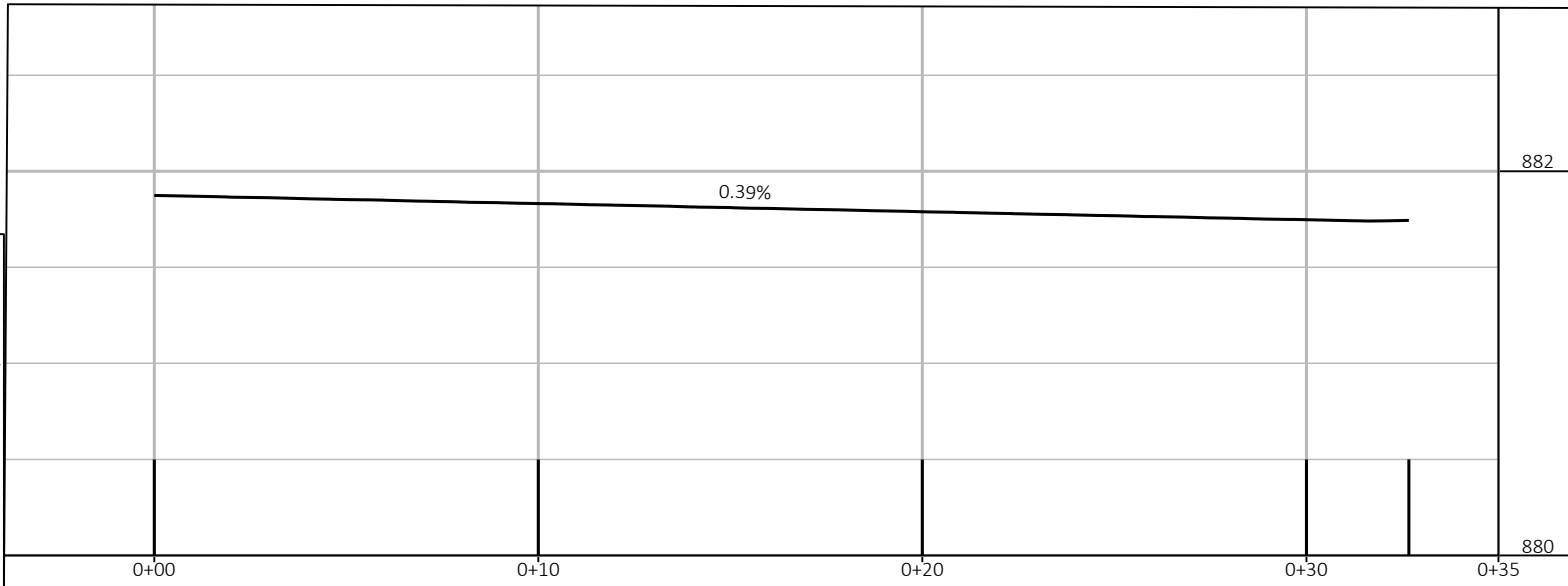
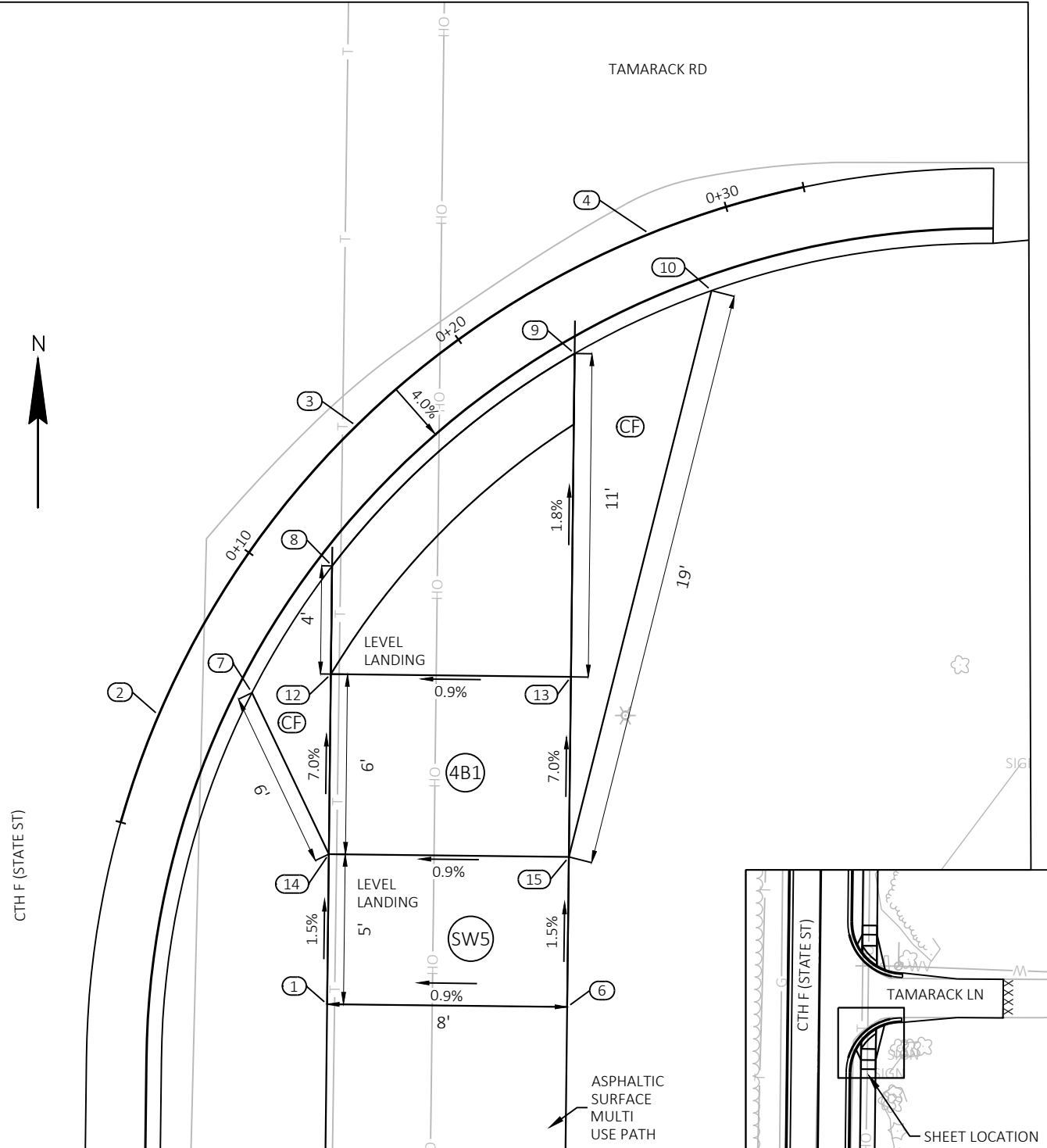
LEGEND	
(X)	CURB RAMP TYPE
(SW5)	CONCRETE SIDEWALK 5-INCH
(XX)	POINT NUMBER
(CF)	CONCRETE FLARE



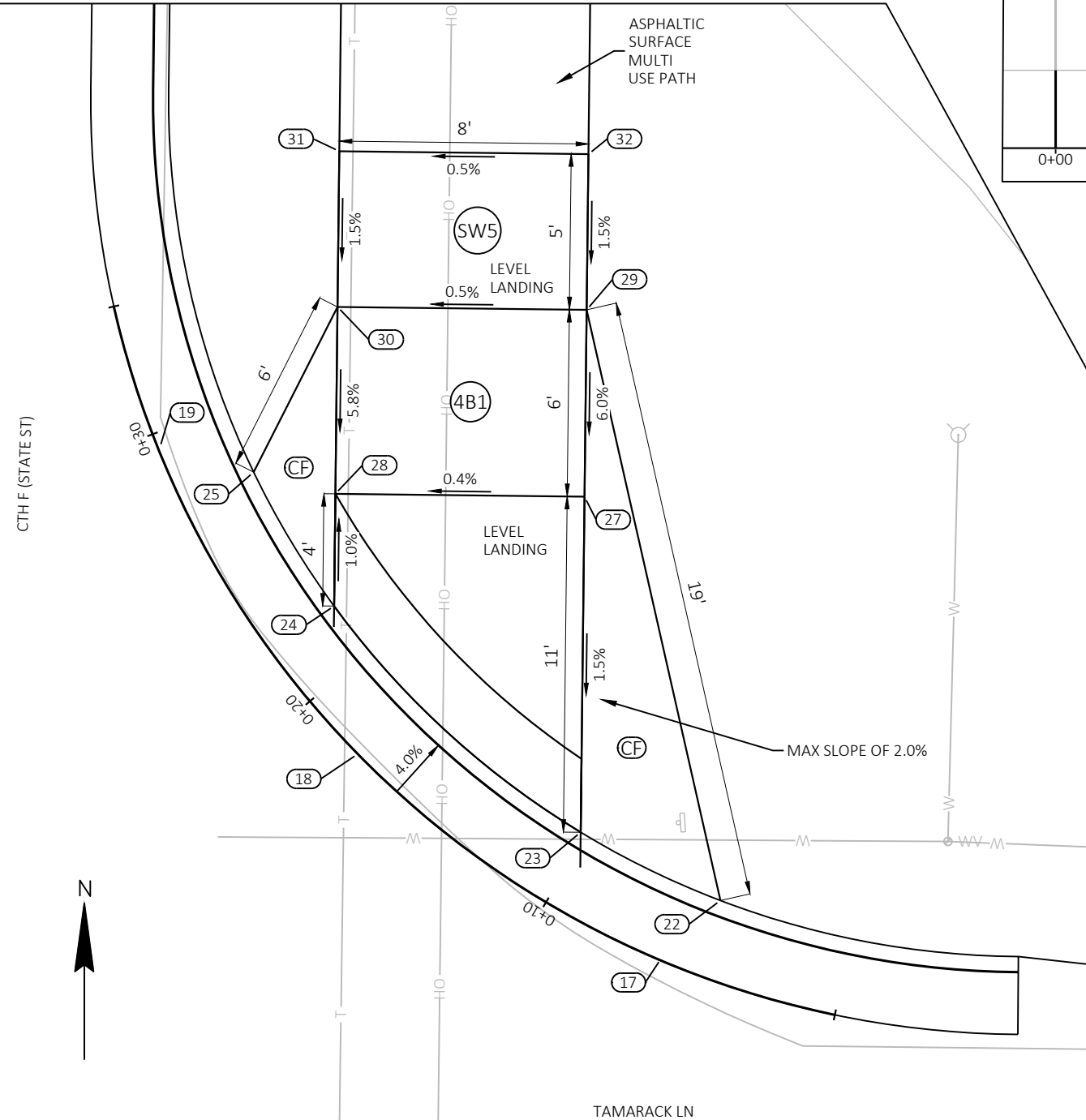
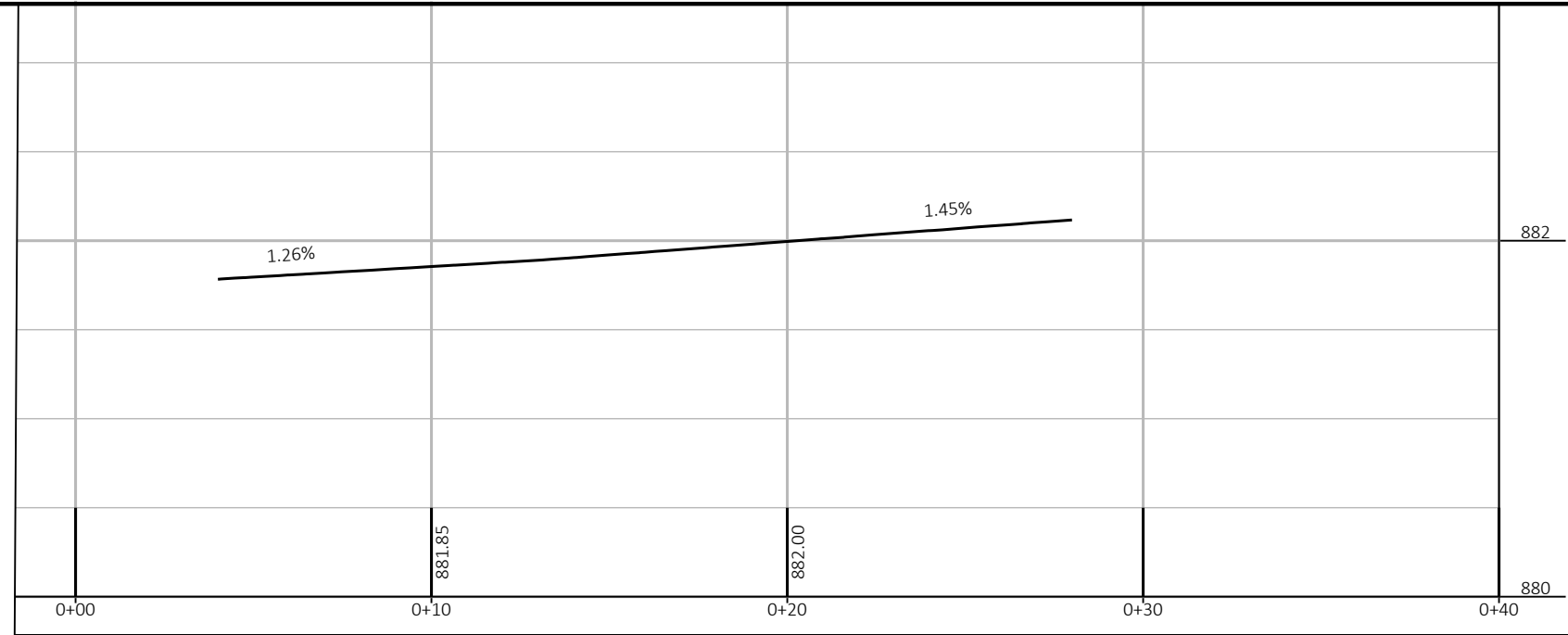
- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES, LENGTHS AND MATCH POINTS PRIOR TO CURB RAMP AND SIDEWALK CONSTRUCTION.
  2. THE ENGINEER MAY ADJUST ELEVATIONS TO FIT FIELD CONSTRUCTION.
  3. SEE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS.
  4. ALL STATION OFFSET INFORMATION REFERENCE CTH F (STATE ST) ALIGNMENT.

- LEGEND
- (X) CURB RAMP TYPE
  - (SW5) CONCRETE SIDEWALK 5-INCH
  - (XX) POINT NUMBER
  - (CF) CONCRETE FLARE

TAMARACK LN SE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1	36+08.95	23.00' RT	882.32	267124.06	340177.29
2	36+18.51	17.28' RT	881.86	267133.69	340171.67
3	36+28.23	23.75' RT	881.81	267143.33	340178.26
4	36+34.74	33.45' RT	881.76	267149.73	340188.03
6	36+08.95	31.00' RT	882.39	267123.97	340185.29
7	36+19.29	20.38' RT	882.27	267134.44	340174.78
8	36+23.55	23.00' RT	881.75	267138.66	340177.46
9	36+30.72	31.00' RT	881.70	267145.74	340185.54
10	36+32.87	35.54' RT	882.17	267147.84	340190.10
12	36+19.95	23.00' RT	881.82	267135.06	340177.41
13	36+19.95	31.00' RT	881.89	267134.96	340185.41
14	36+13.95	23.00' RT	882.24	267129.06	340177.35
15	36+13.95	31.00' RT	882.31	267128.97	340185.35

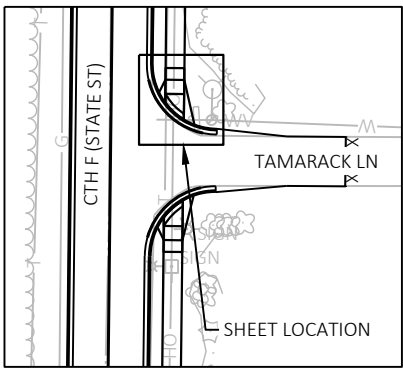


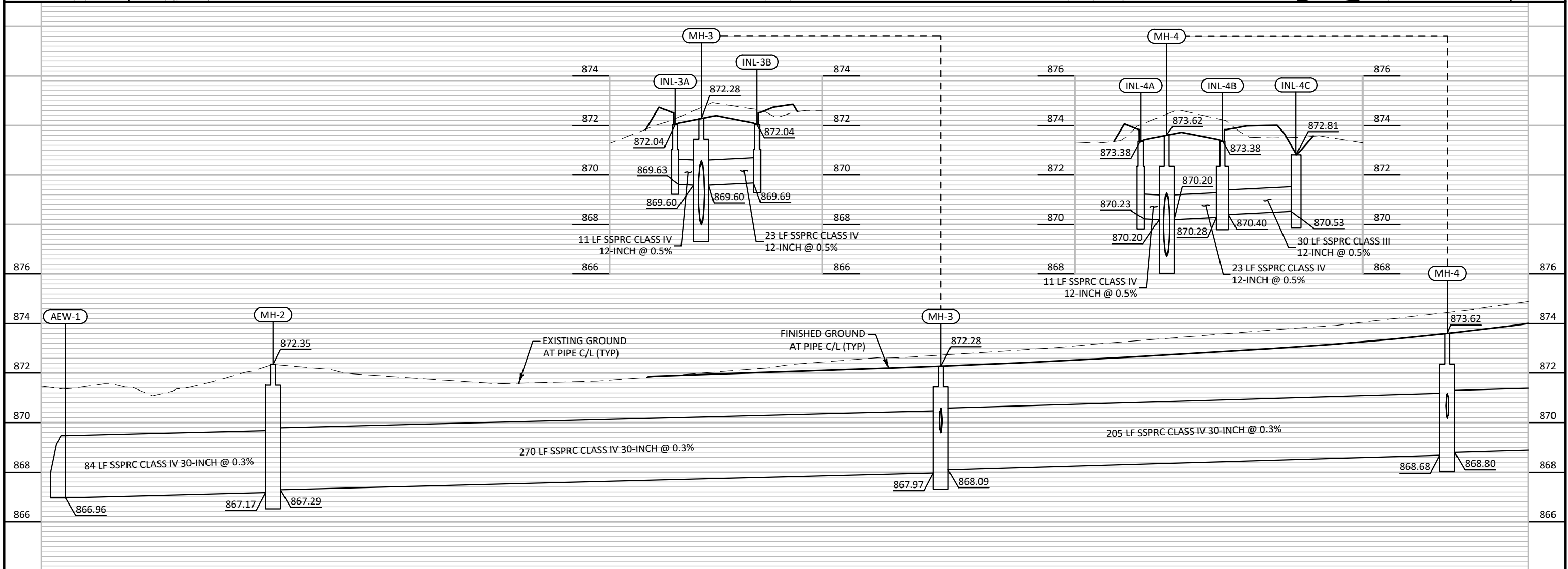
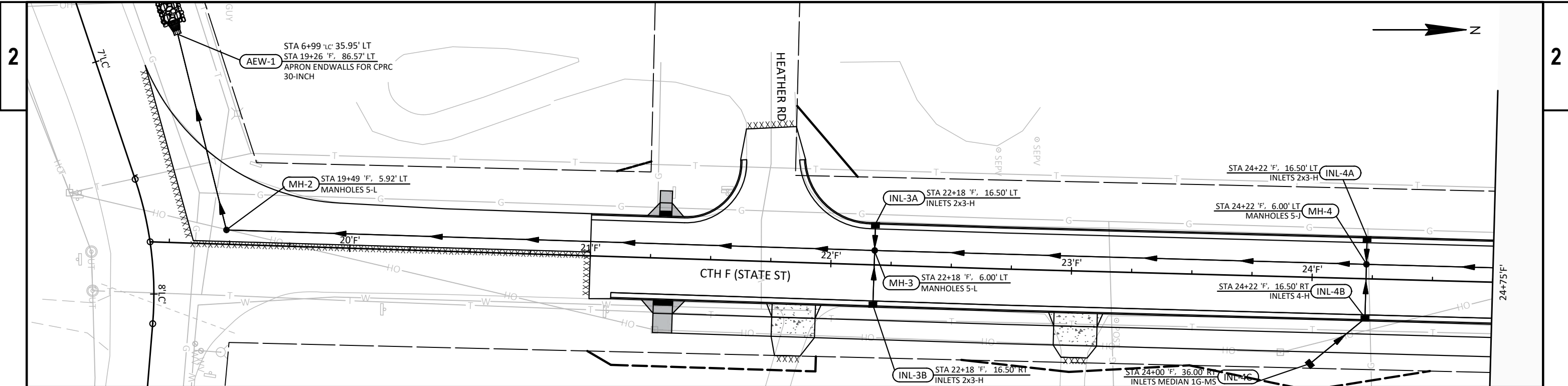
- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES, LENGTHS AND MATCH POINTS PRIOR TO CURB RAMP AND SIDEWALK CONSTRUCTION.
  2. THE ENGINEER MAY ADJUST ELEVATIONS TO FIT FIELD CONSTRUCTION.
  3. SEE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS.
  4. ALL STATION OFFSET INFORMATION REFERENCE CTH F (STATE ST) ALIGNMENT.



- LEGEND**
- (X) CURB RAMP TYPE
  - (SW5) CONCRETE SIDEWALK 5-INCH
  - (XX) POINT NUMBER
  - (CF) CONCRETE FLARE

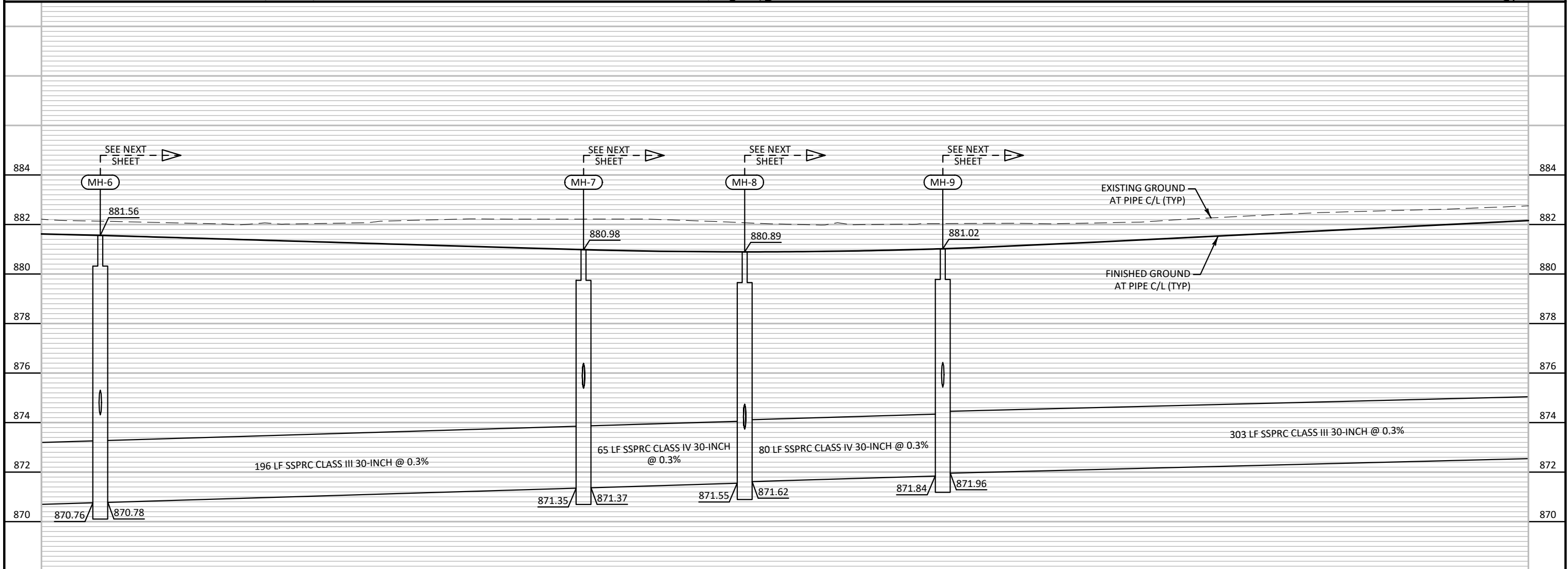
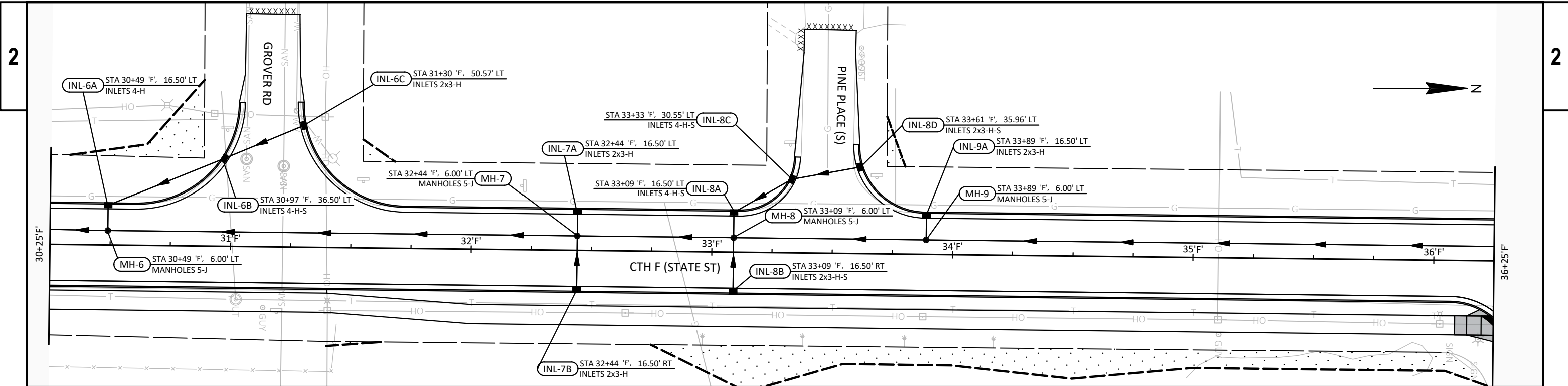
TAMARACK LN NE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
17	36+61.35	33.62' RT	881.80	267176.34	340188.51
18	36+67.81	23.82' RT	881.96	267182.91	340178.78
19	36+77.57	17.34' RT	882.14	267192.74	340172.42
22	36+63.24	35.51' RT	882.21	267178.21	340190.42
23	36+65.38	31.00' RT	882.28	267180.40	340185.94
24	36+72.55	23.00' RT	882.45	267187.66	340178.02
25	36+76.82	20.37' RT	882.53	267191.96	340175.44
27	36+76.16	31.00' RT	881.94	267191.17	340186.06
28	36+76.16	23.00' RT	881.91	267191.27	340178.06
29	36+82.16	31.00' RT	882.30	267197.17	340186.13
30	36+82.16	23.00' RT	882.26	267197.26	340178.13
31	36+87.16	23.00' RT	882.33	267202.26	340178.19
32	36+87.16	31.00' RT	882.37	267202.17	340186.19



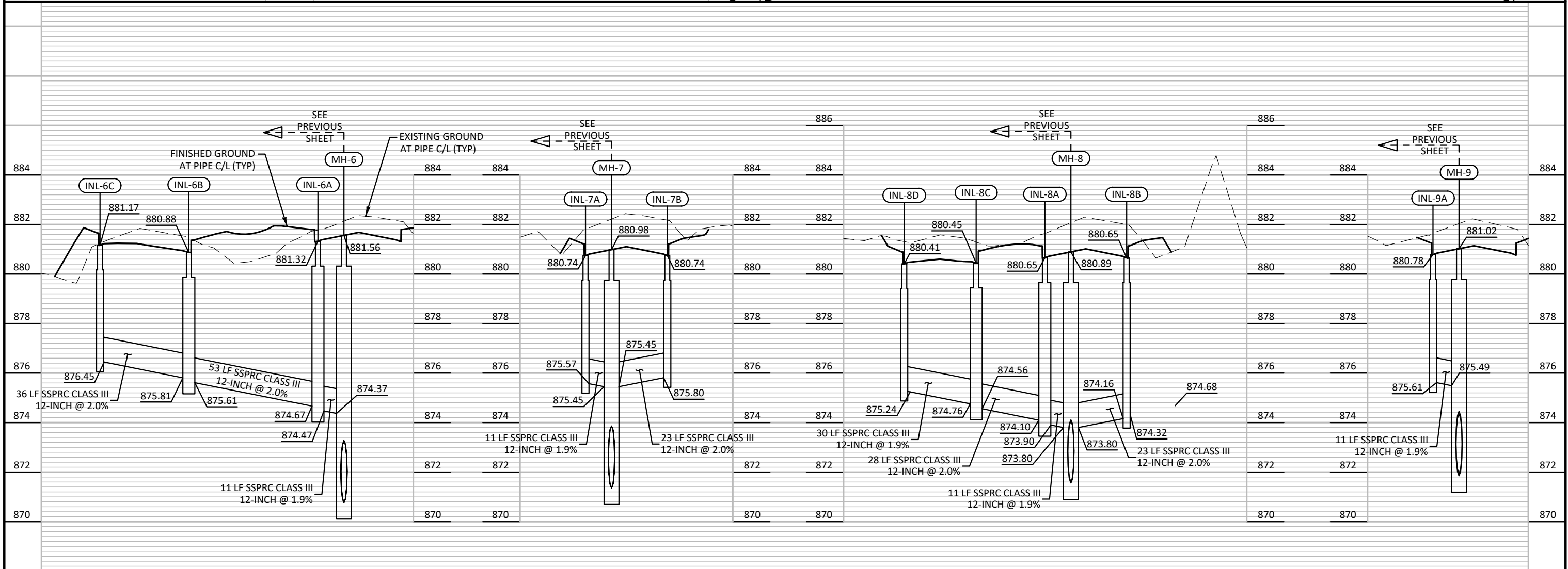
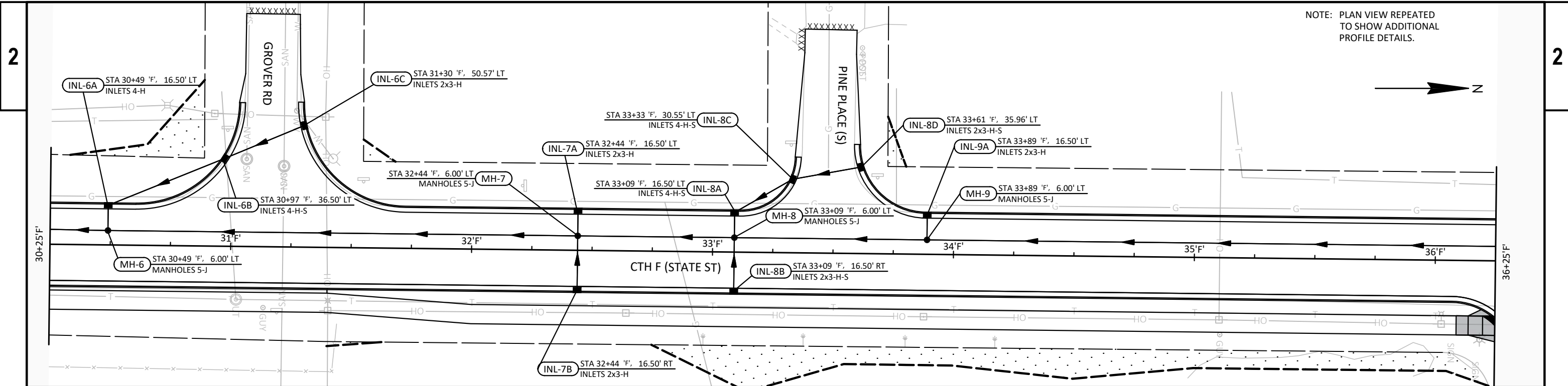




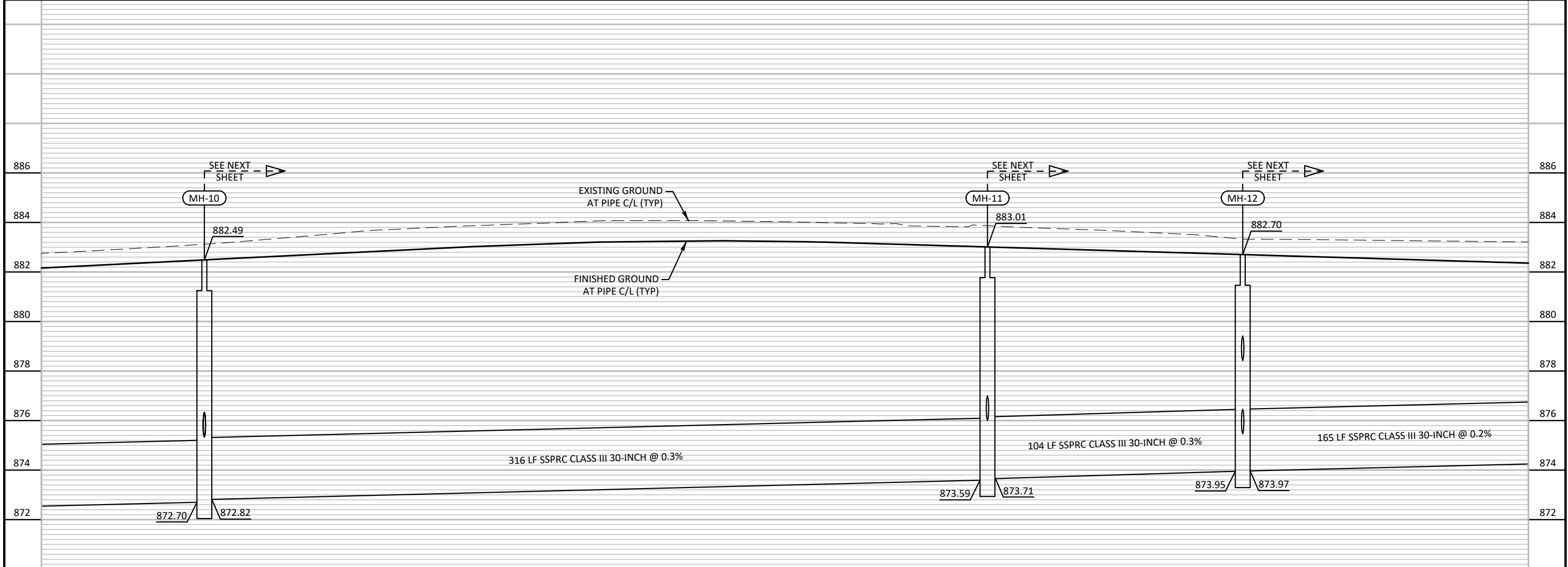
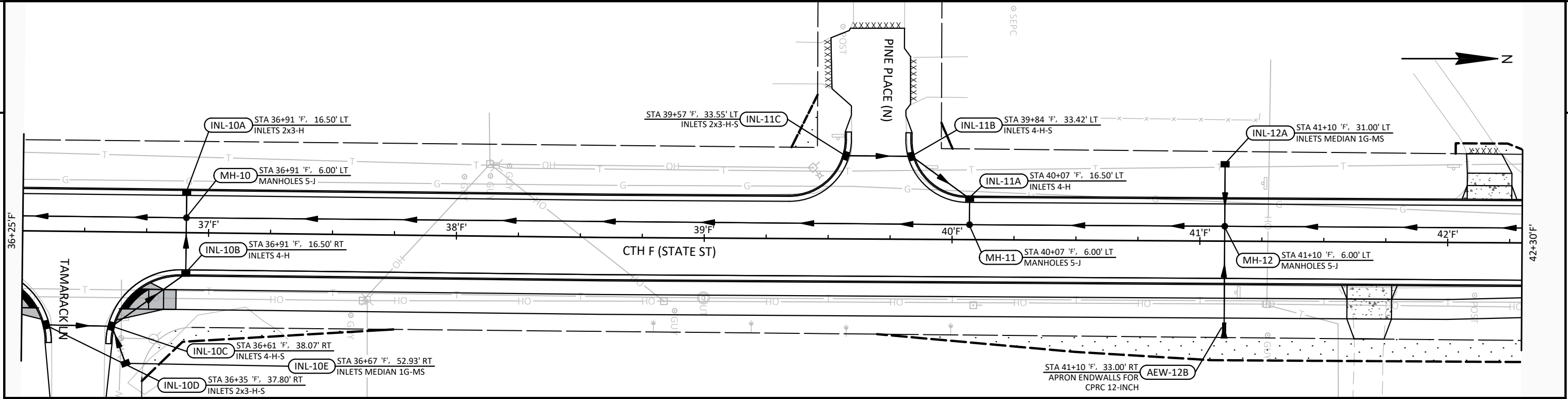




PROJECT NO: 7995-02-63      HWY: CTH F      COUNTY: EAU CLAIRE      STORM SEWER DETAILS      SHEET      E

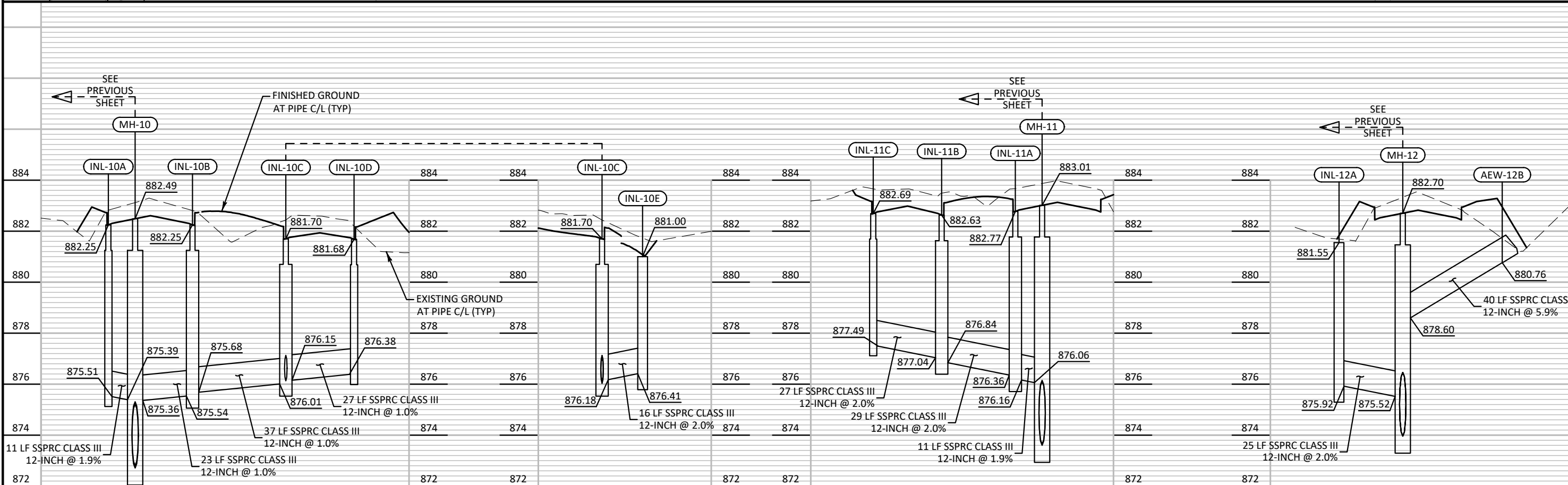
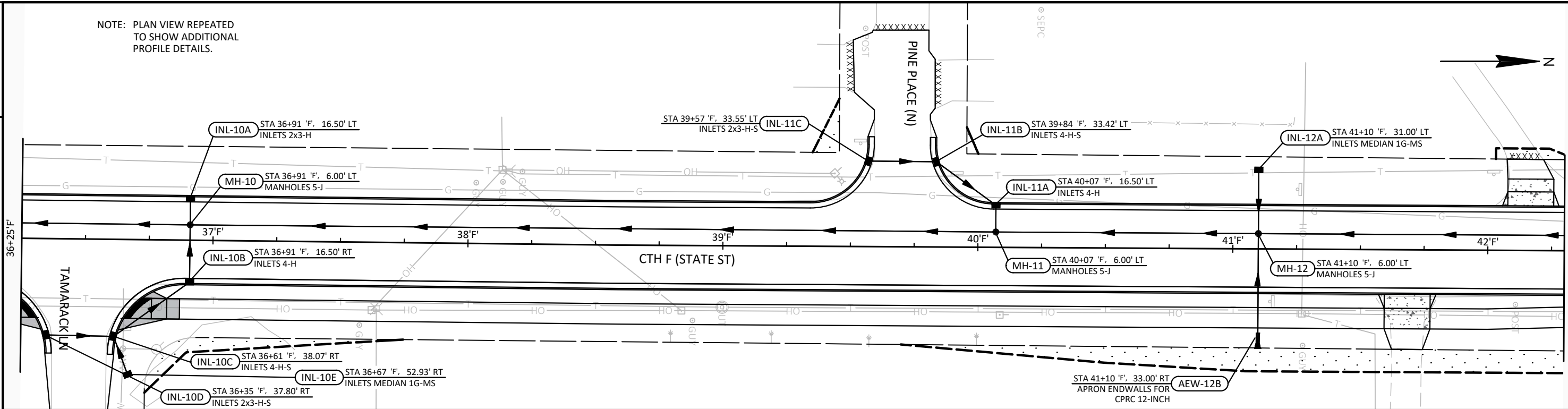


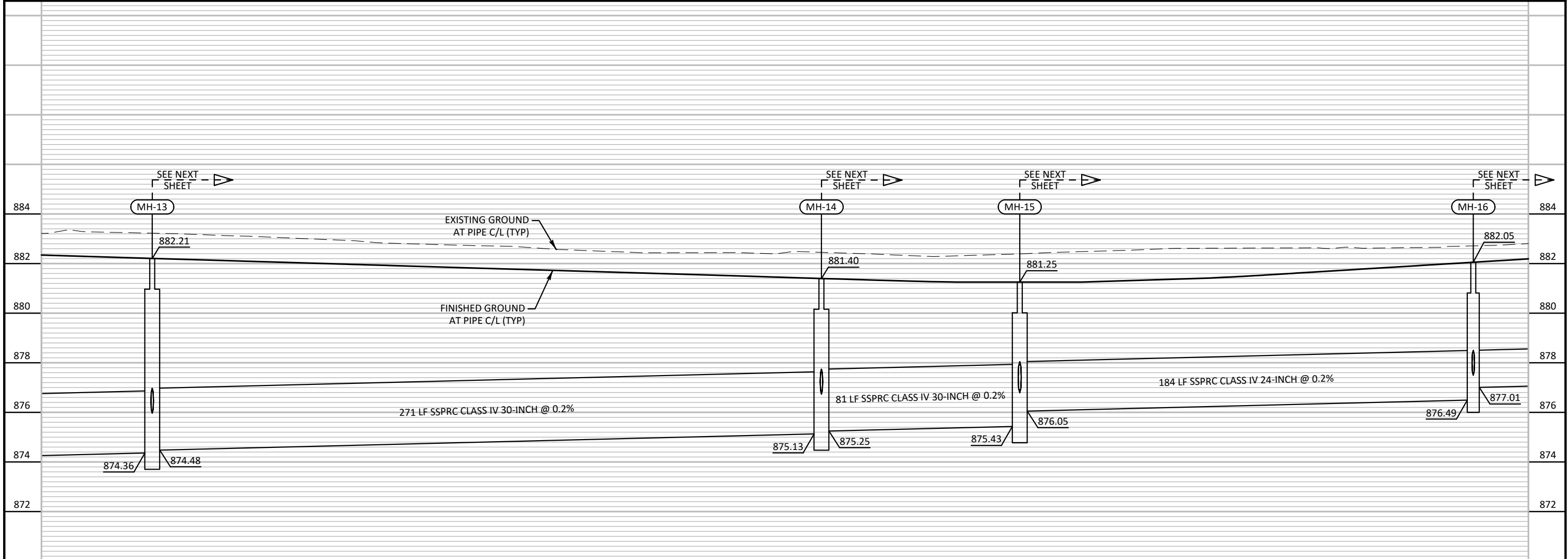
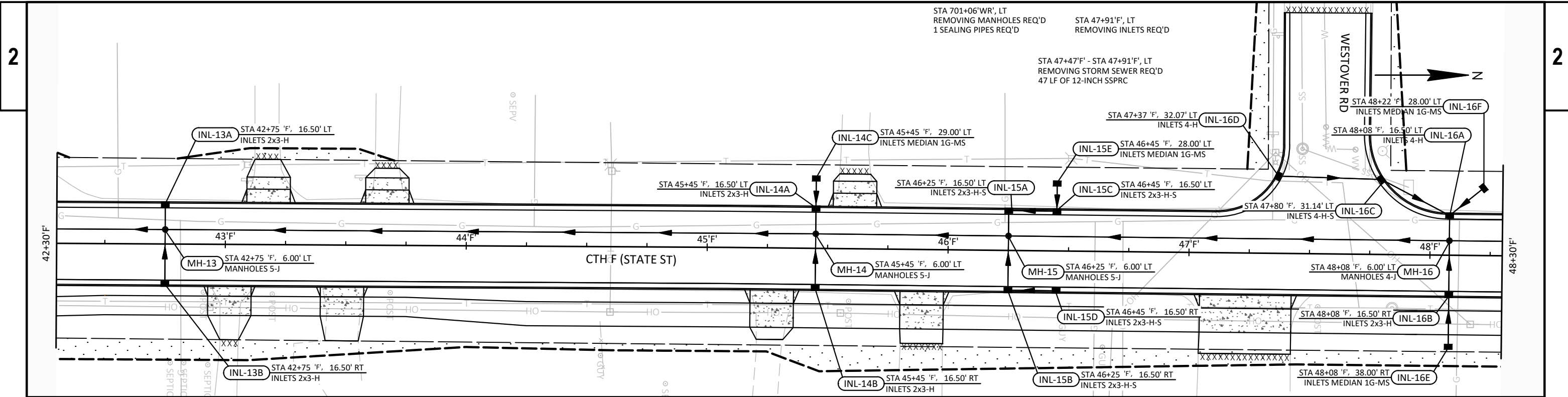
PROJECT NO: 7995-02-63      HWY: CTH F      COUNTY: EAU CLAIRE      STORM SEWER DETAILS      SHEET      E



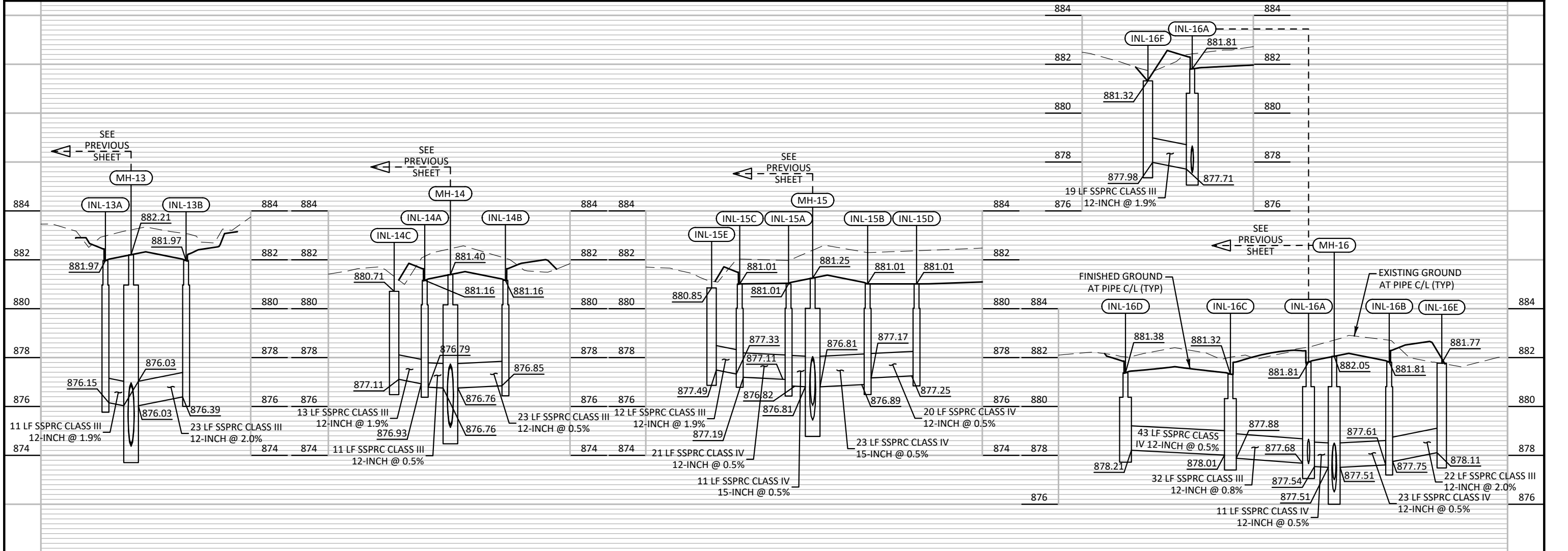
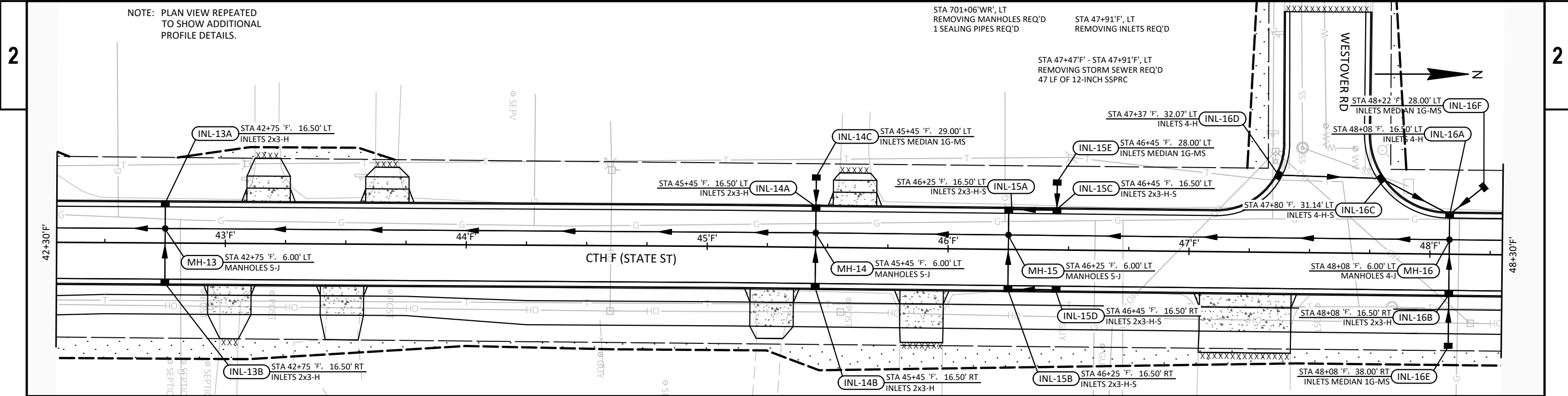
PROJECT NO: 7995-02-63	HWY: CTH F	COUNTY: EAU CLAIRE	STORM SEWER DETAILS	SHEET	E
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NOTE: PLAN VIEW REPEATED TO SHOW ADDITIONAL PROFILE DETAILS.

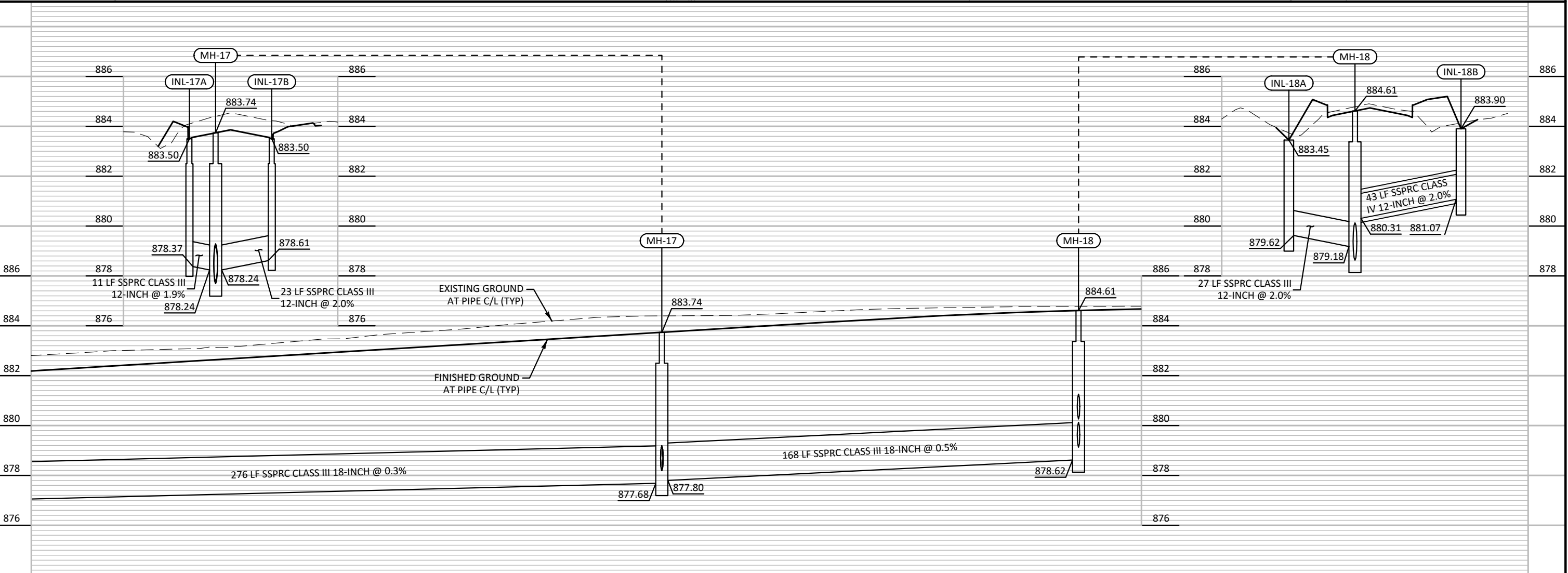
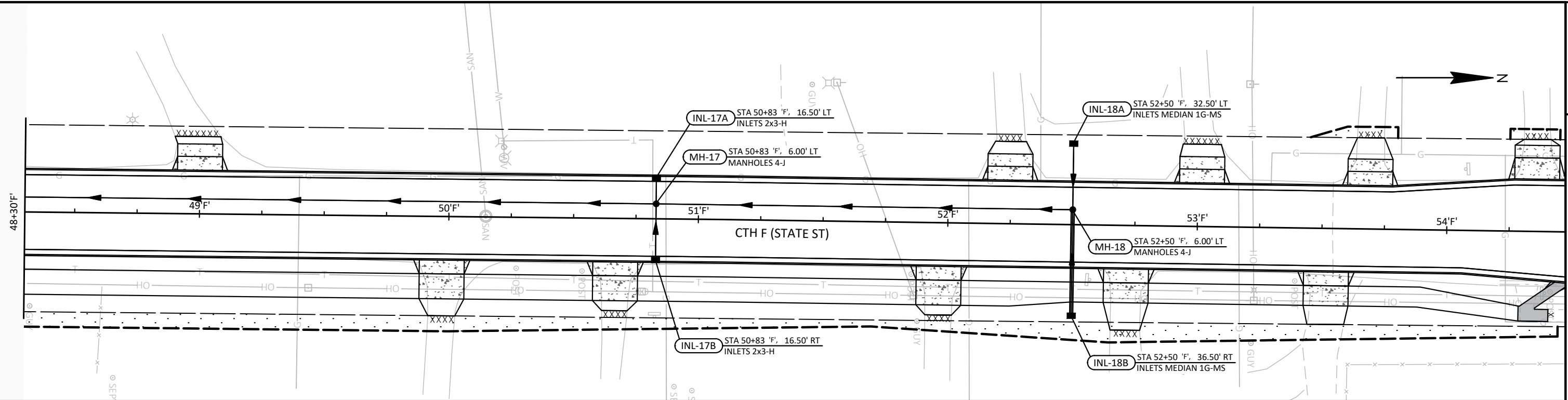


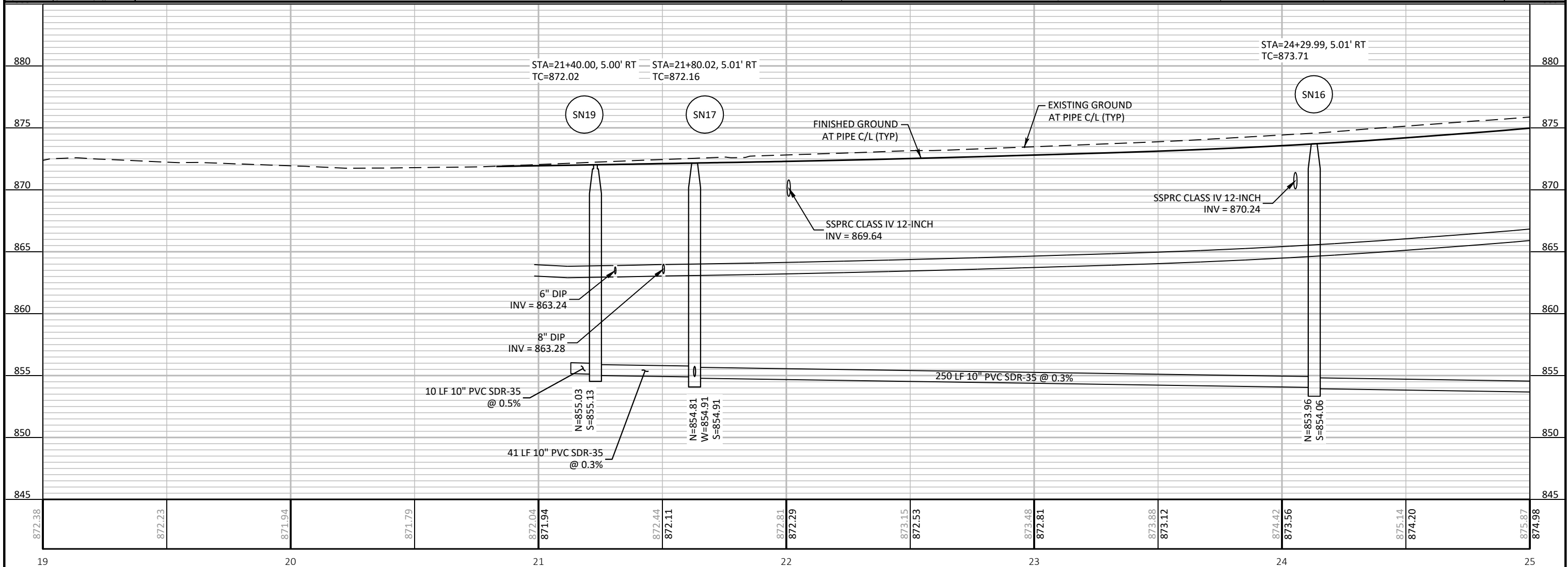
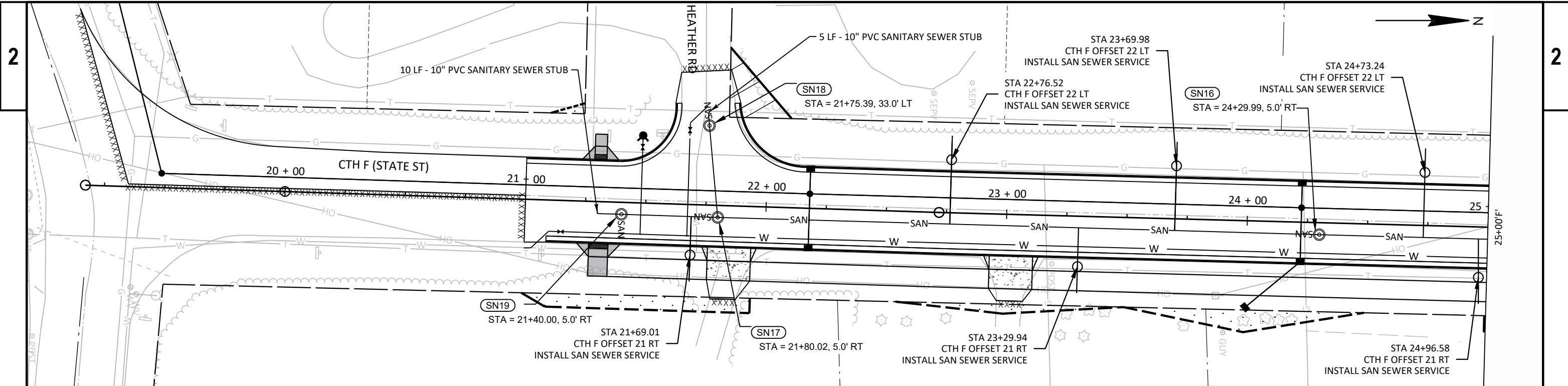


PROJECT NO: 7995-02-63	HWY: CTH F	COUNTY: EAU CLAIRE	STORM SEWER DETAILS	SHEET	E
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PROJECT NO: 7995-02-63	HWY: CTH F	COUNTY: EAU CLAIRE	STORM SEWER DETAILS	SHEET	E
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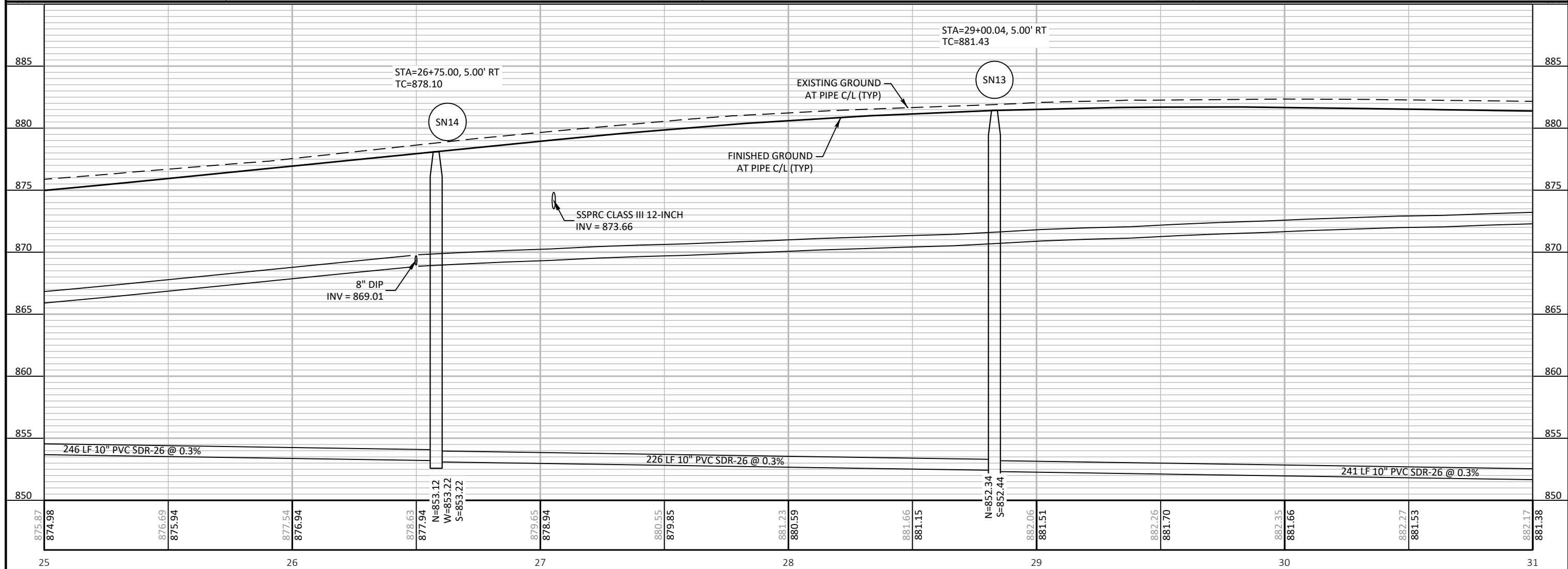
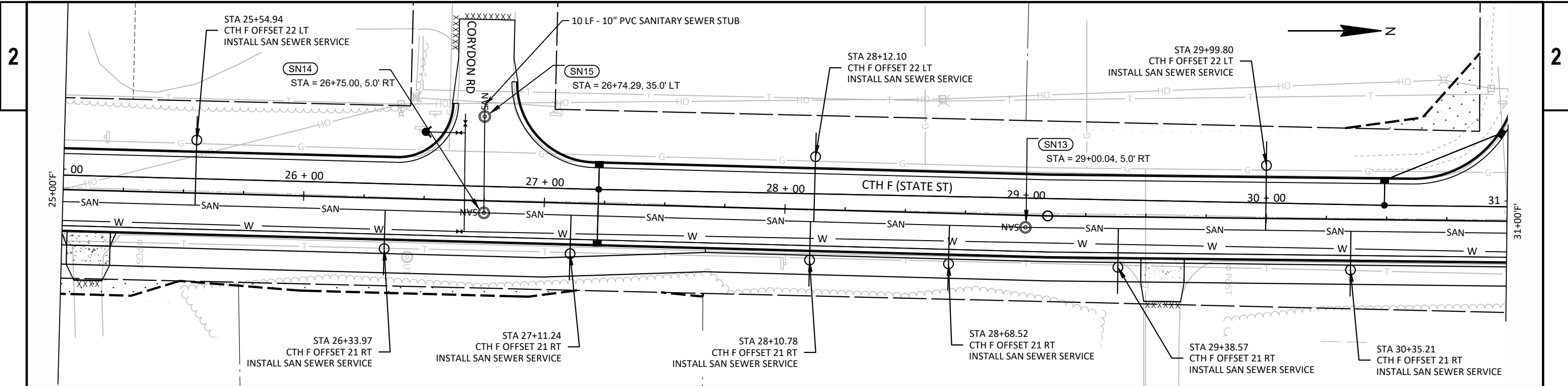




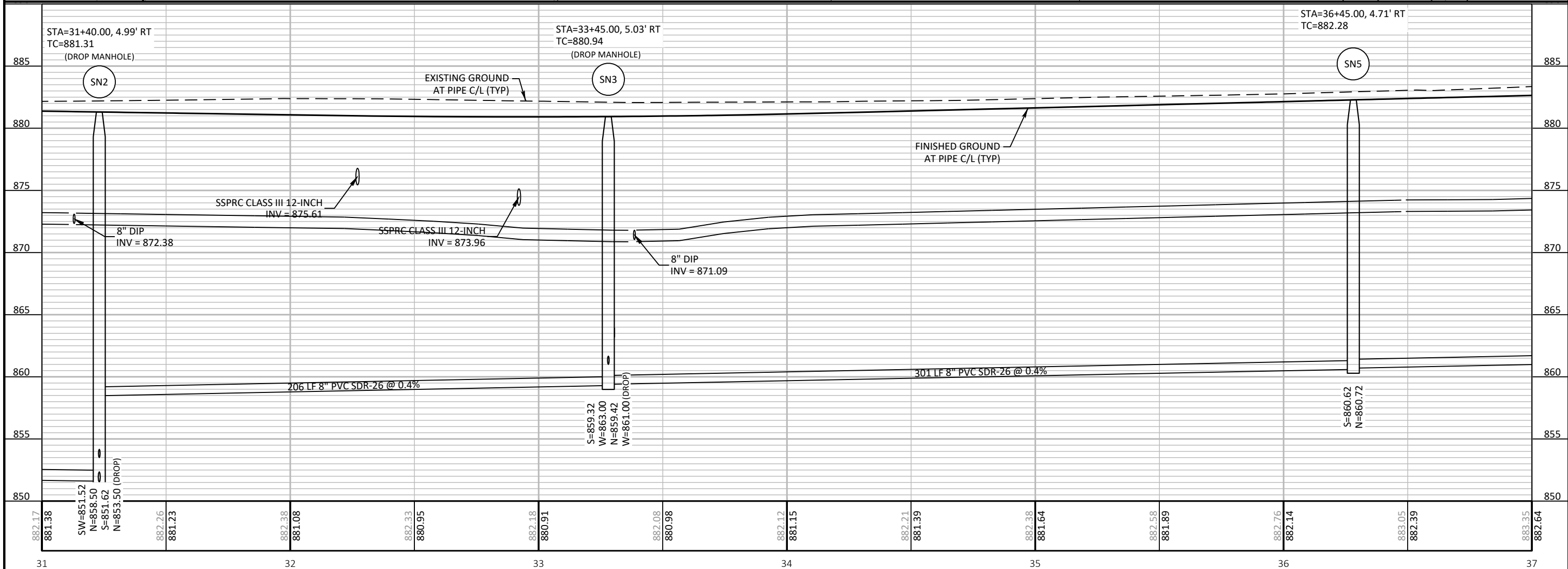
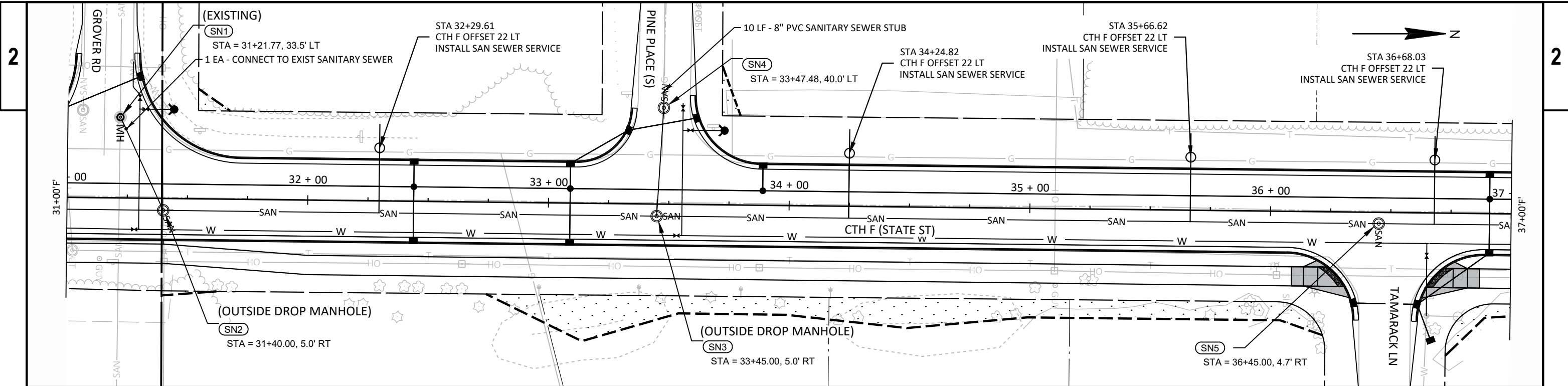
19	20	21	22	23	24	25
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PROJECT NO: 7995-02-63      HWY: CTH F      COUNTY: EAU CLAIRE      SANITARY SEWER DETAILS      SHEET      E

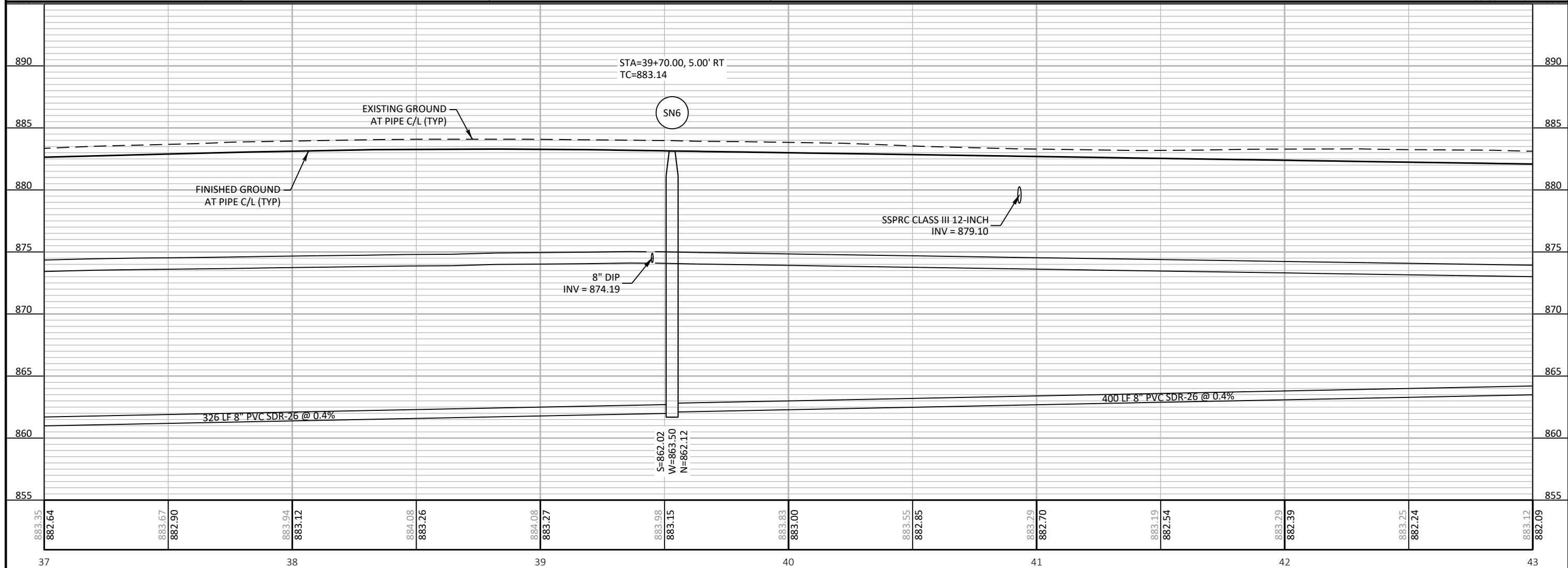
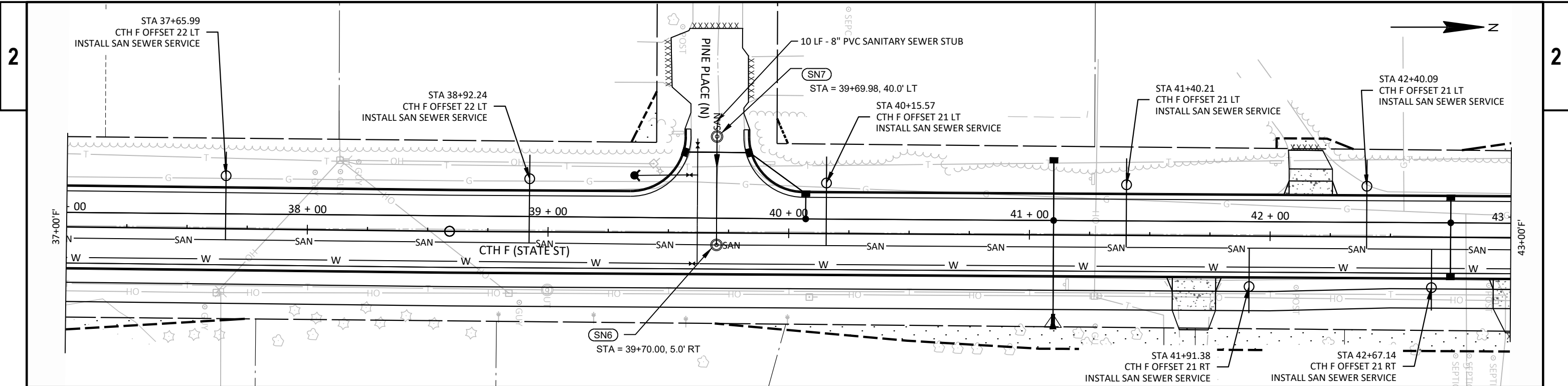




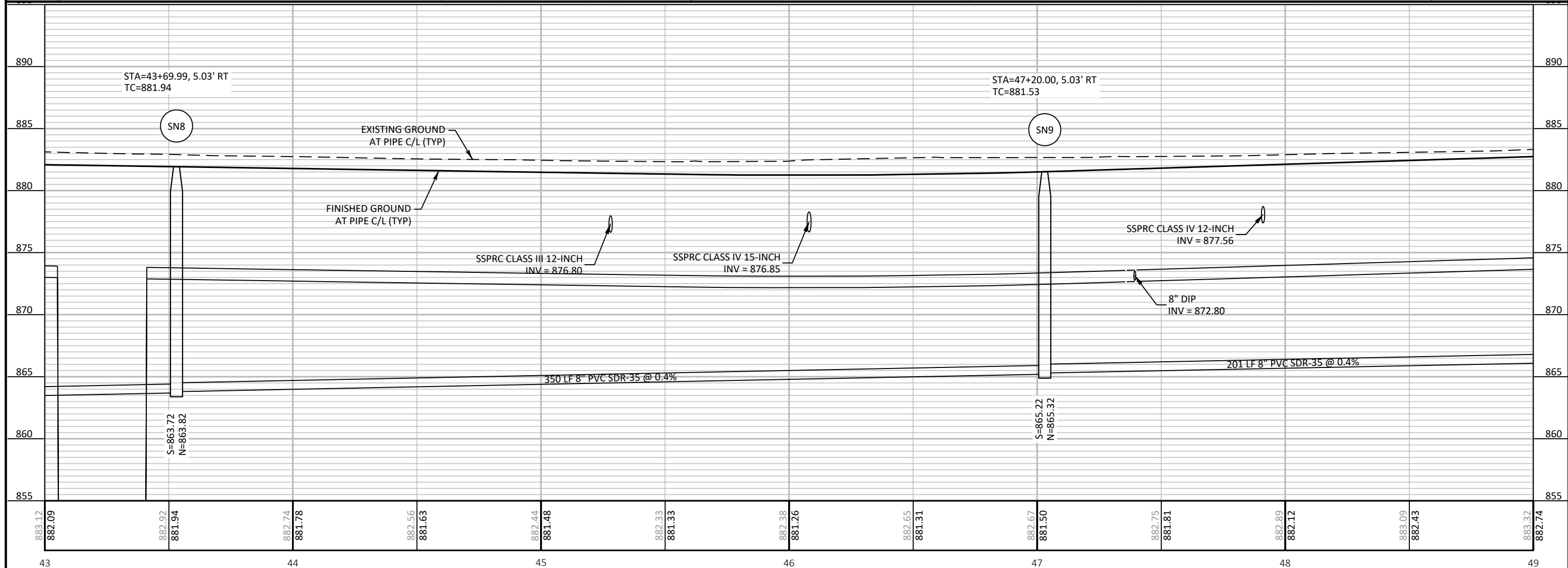
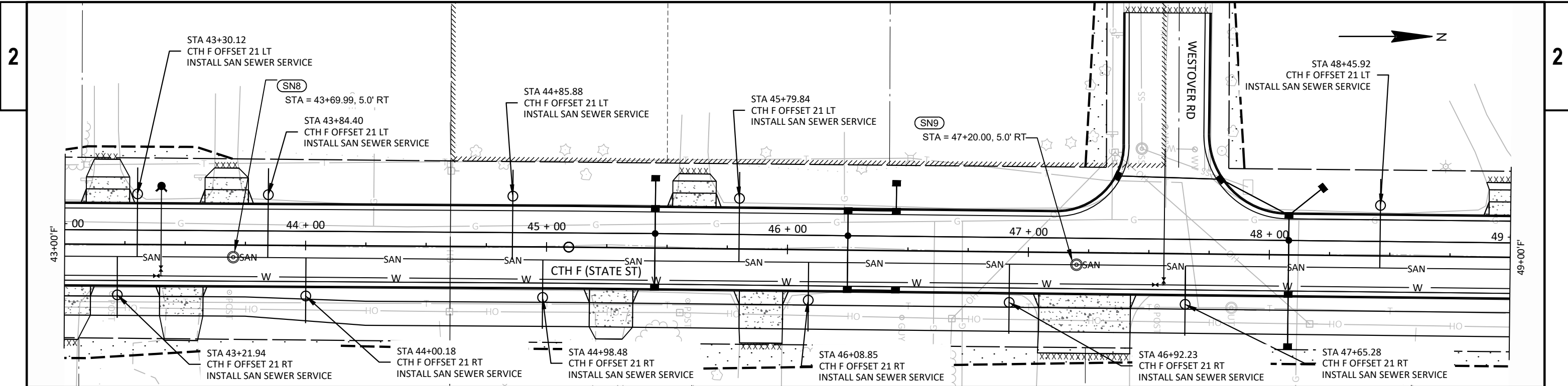
PROJECT NO: 7995-02-63	HWY: CTH F	COUNTY: EAU CLAIRE	SANITARY SEWER DETAILS	SHEET	E
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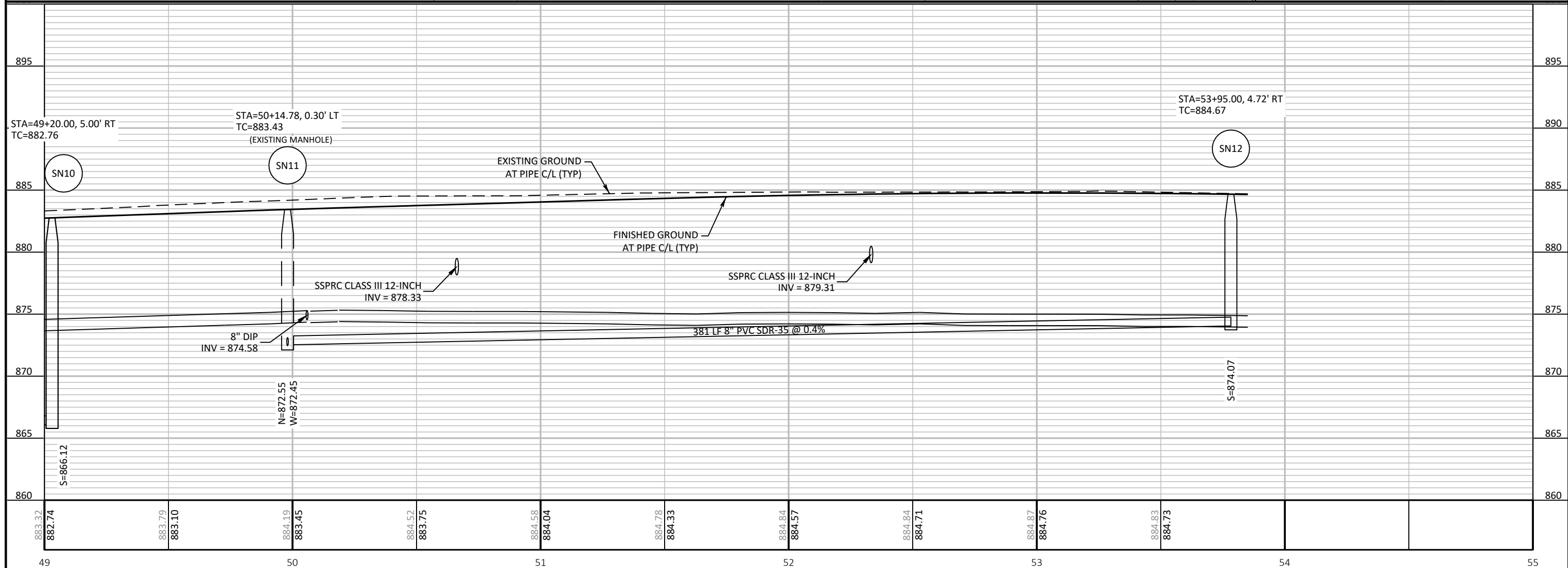
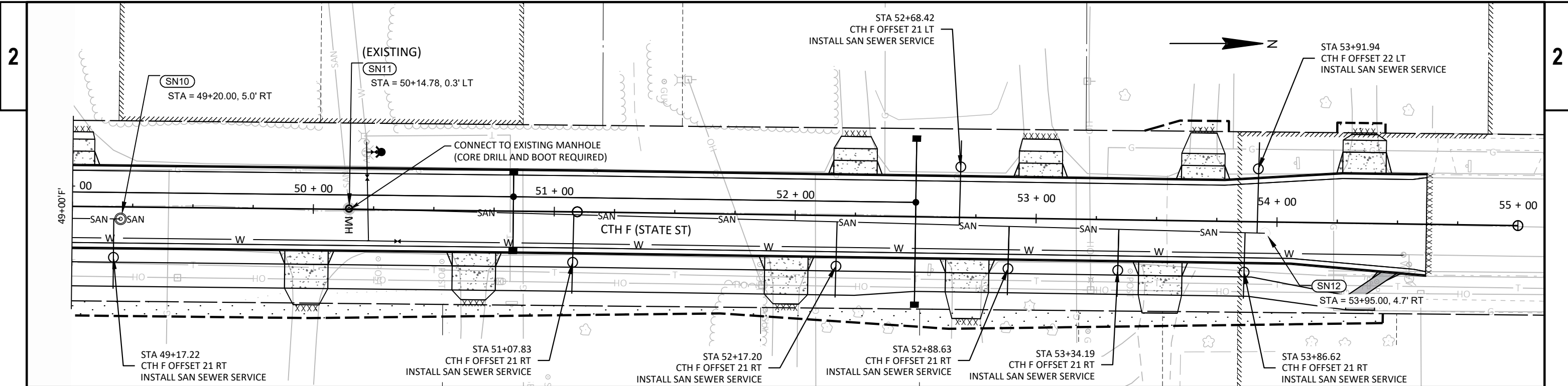
PROJECT NO: 7995-02-63	HWY: CTH F	COUNTY: EAU CLAIRE	SANITARY SEWER DETAILS	SHEET	E
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PROJECT NO:	7995-02-63	HWY:	CTH F	COUNTY:	EAU CLAIRE	SANITARY SEWER DETAILS	SHEET	E
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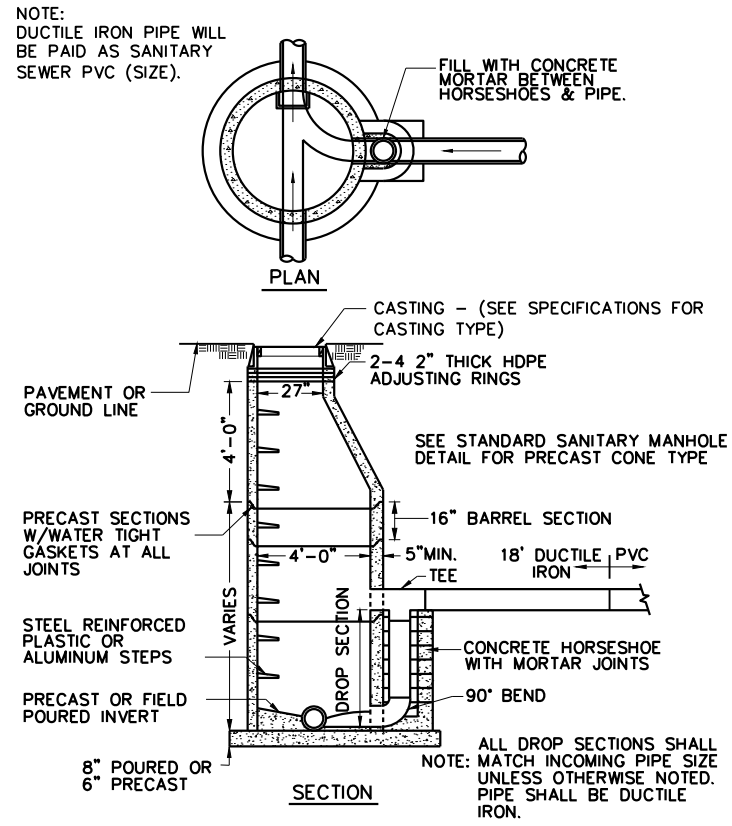


PROJECT NO: 7995-02-63	HWY: CTH F	COUNTY: EAU CLAIRE	SANITARY SEWER DETAILS	SHEET	E
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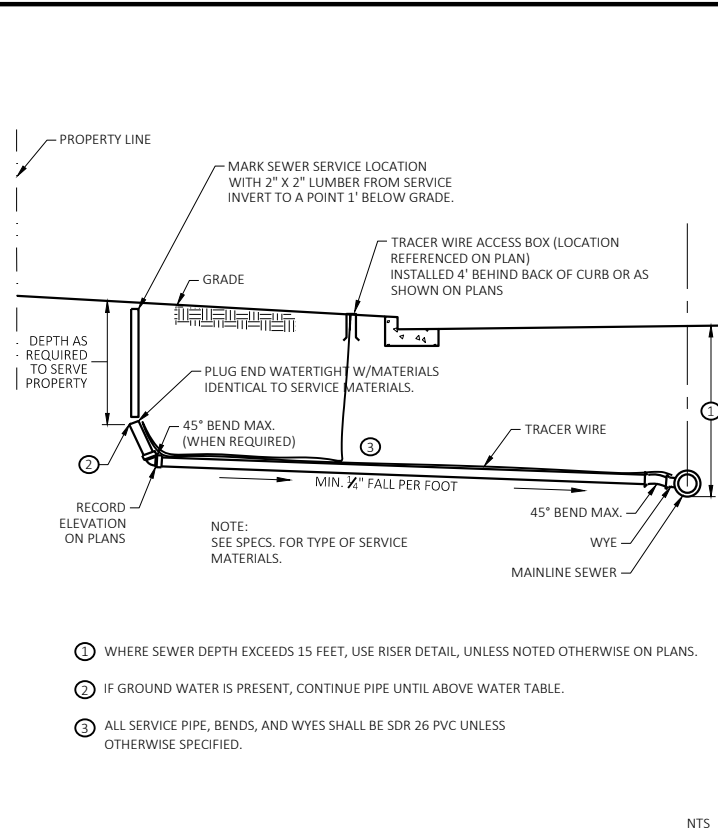


PROJECT NO:	7995-02-63	HWY:	CTH F	COUNTY:	EAU CLAIRE	SANITARY SEWER DETAILS	SHEET	E
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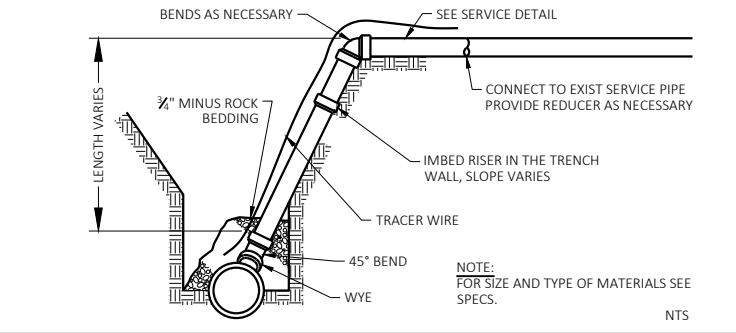


SANITARY DROP MH (OUTSIDE)

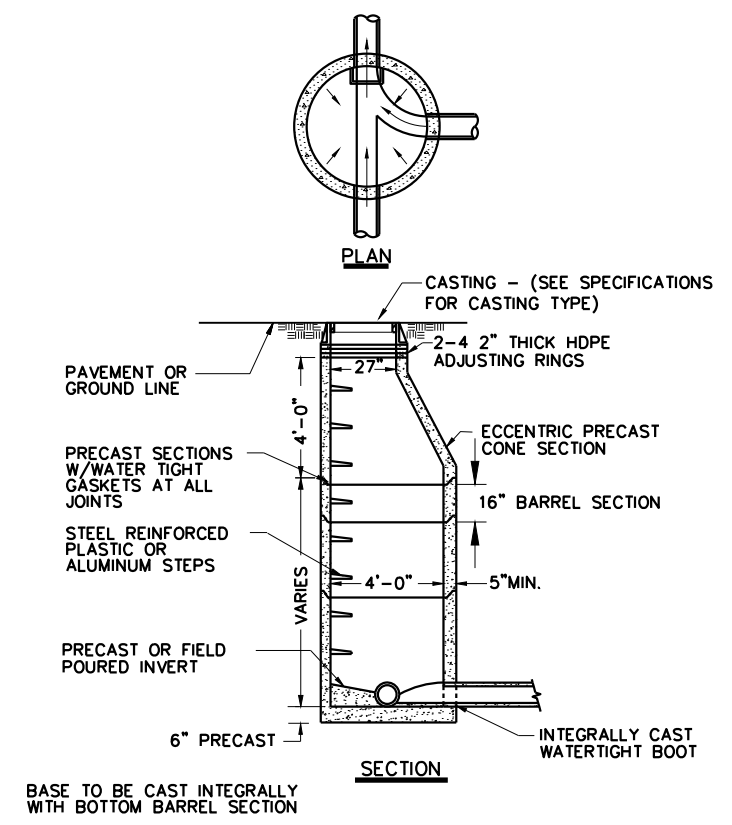


SANITARY SEWER SERVICE

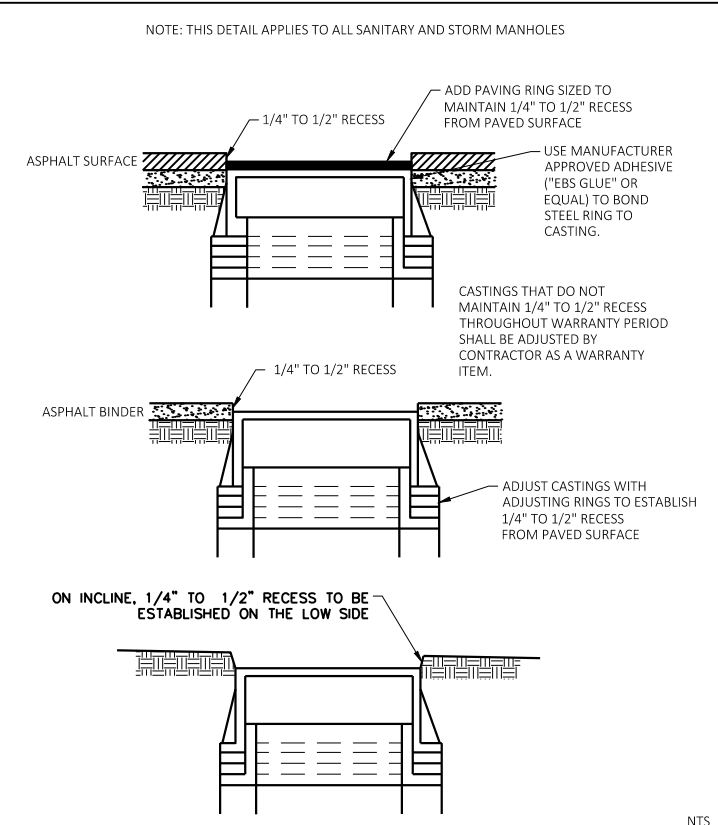
Revised: Oct. 2023  
SEH Plate No. SAN-11



SANITARY SEWER SERVICE RISER

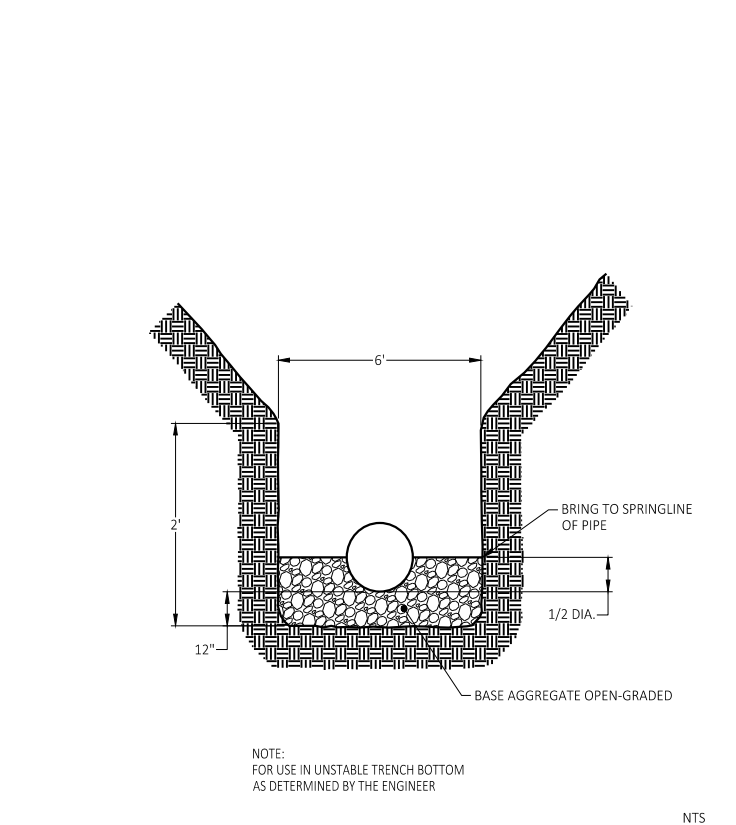


SANITARY MANHOLE



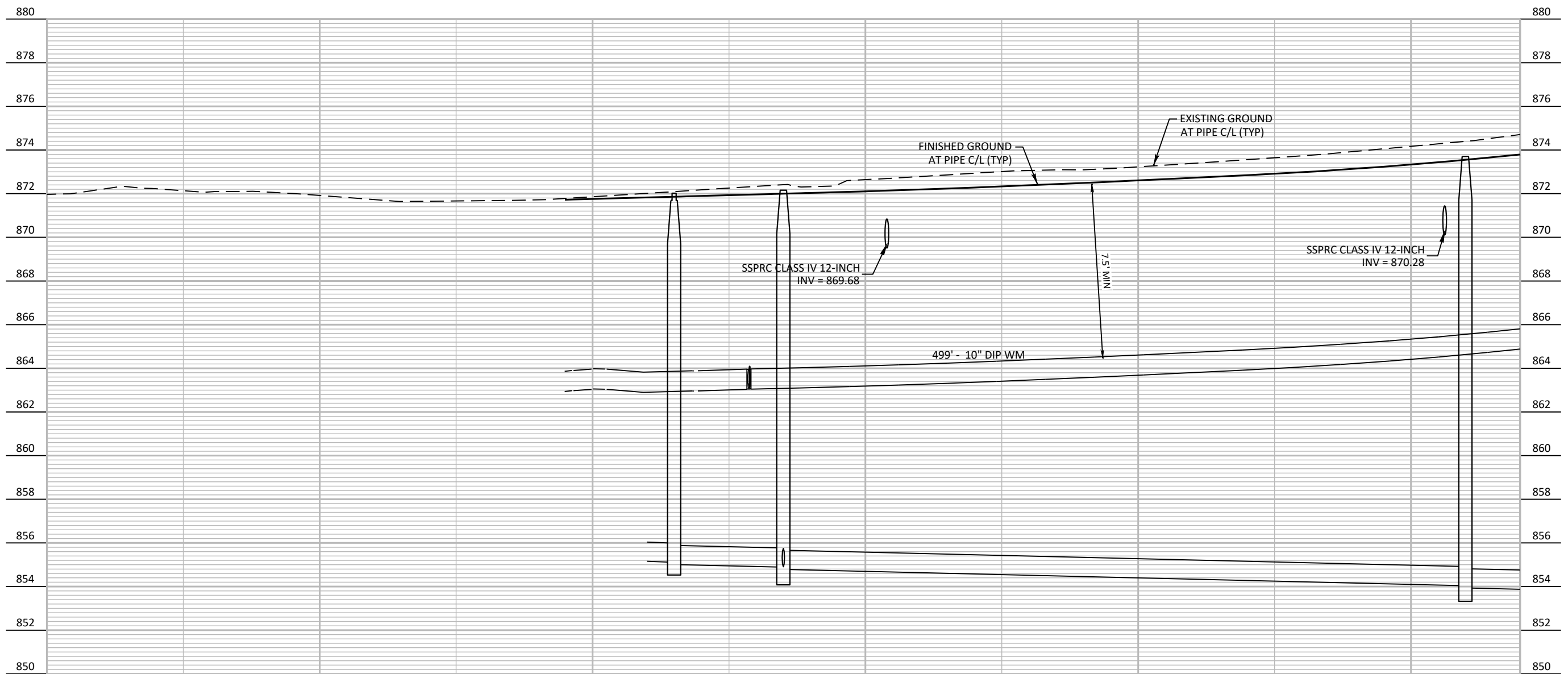
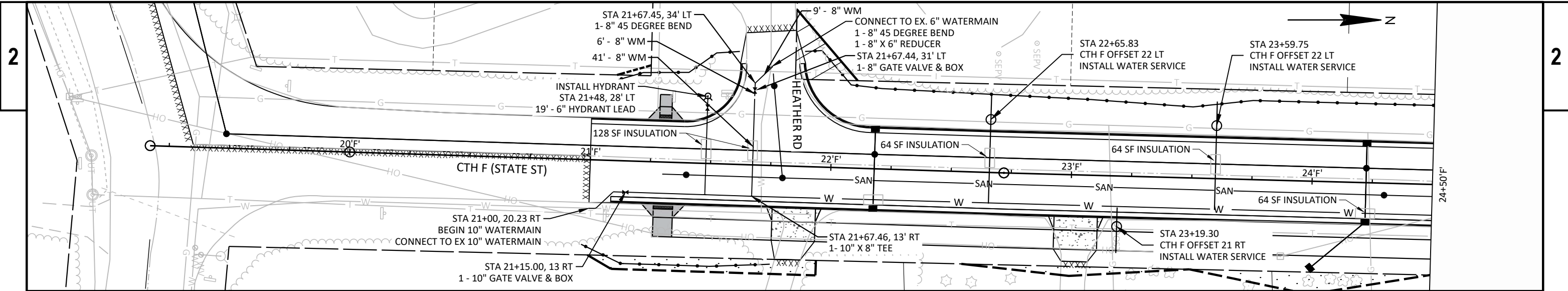
MANHOLE CASTING ADJUSTMENT

Revised: Oct. 2011  
SEH Plate No. SAN-16



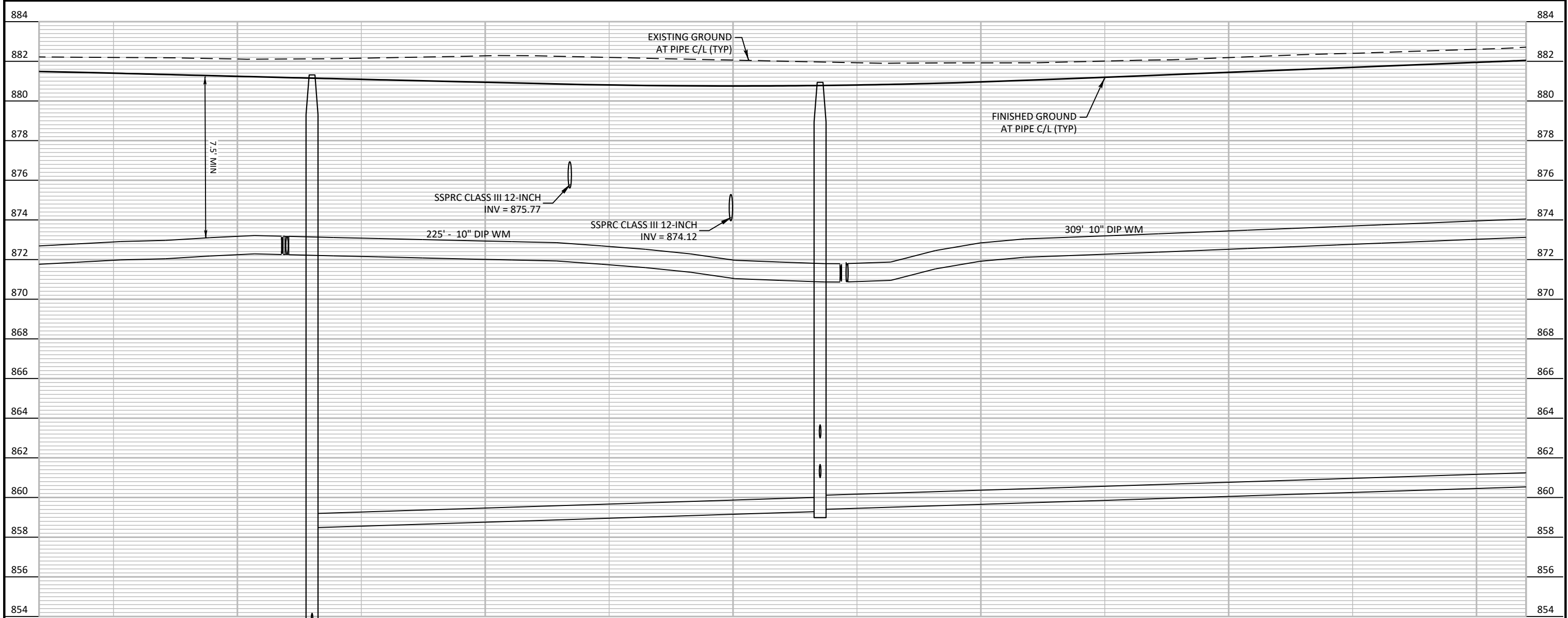
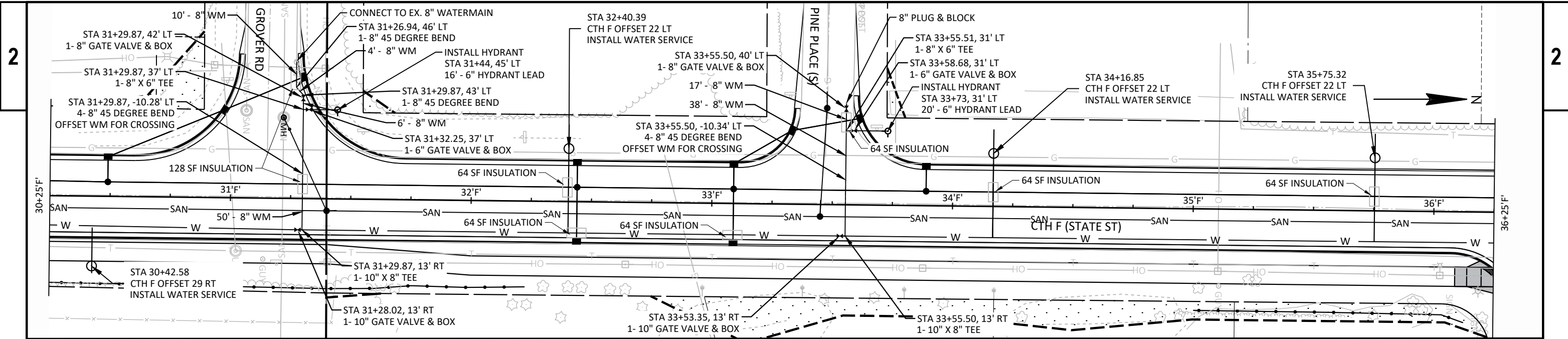
TRENCH BEDDING AND FOUNDATION

Revised: Oct. 2016  
SEH Plate No. SAN-15

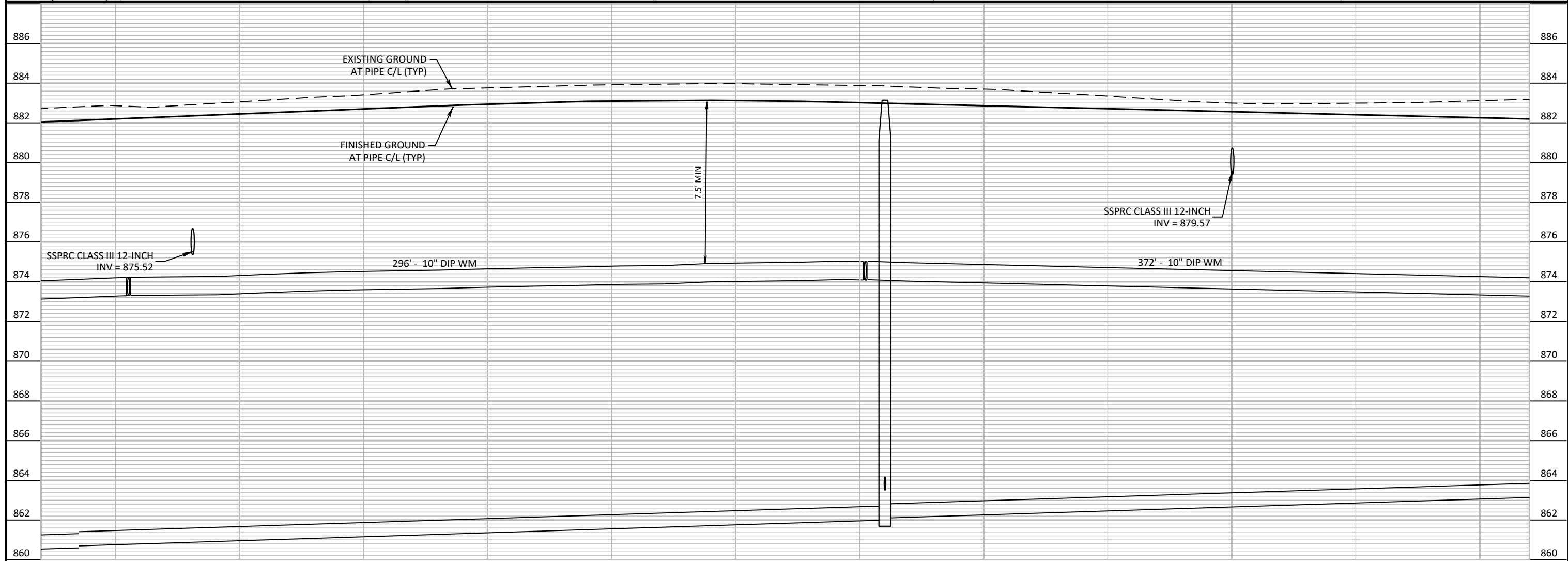
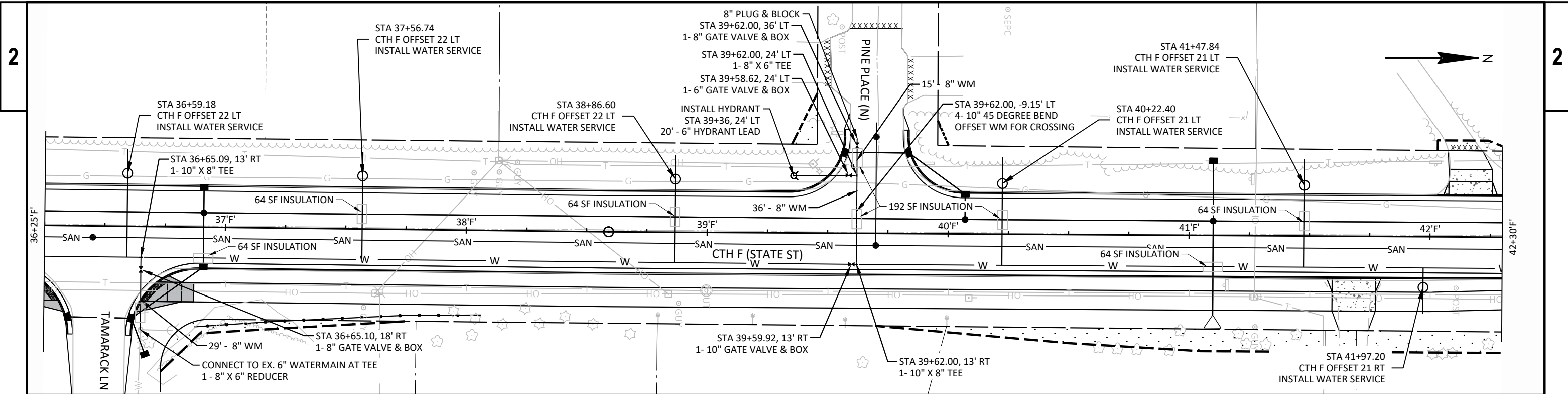




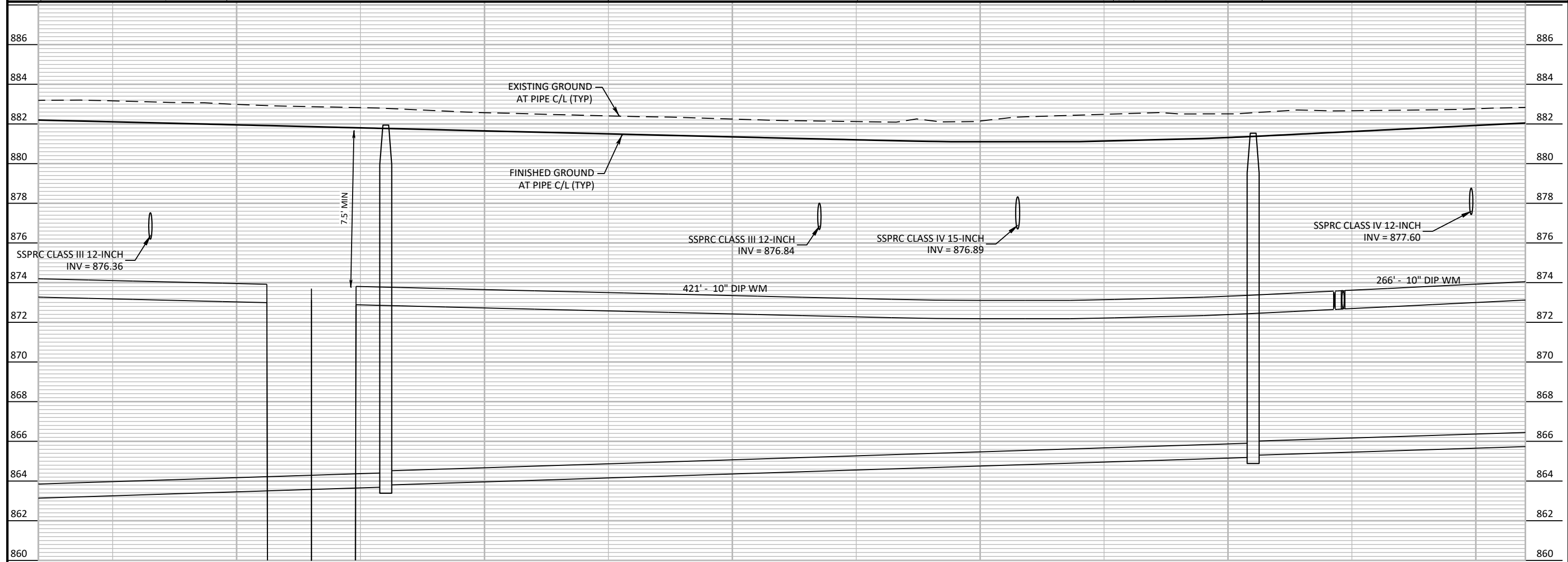
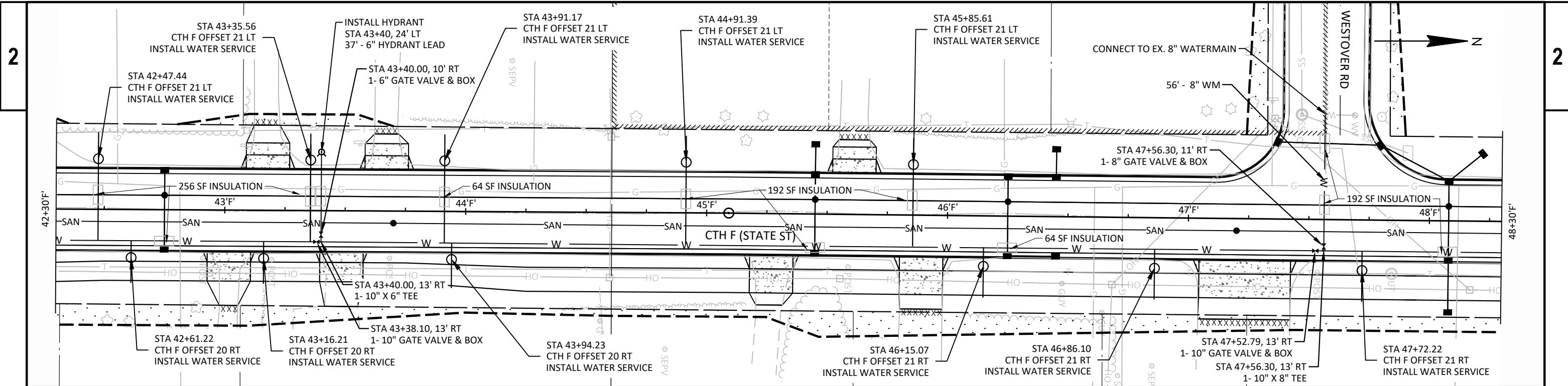


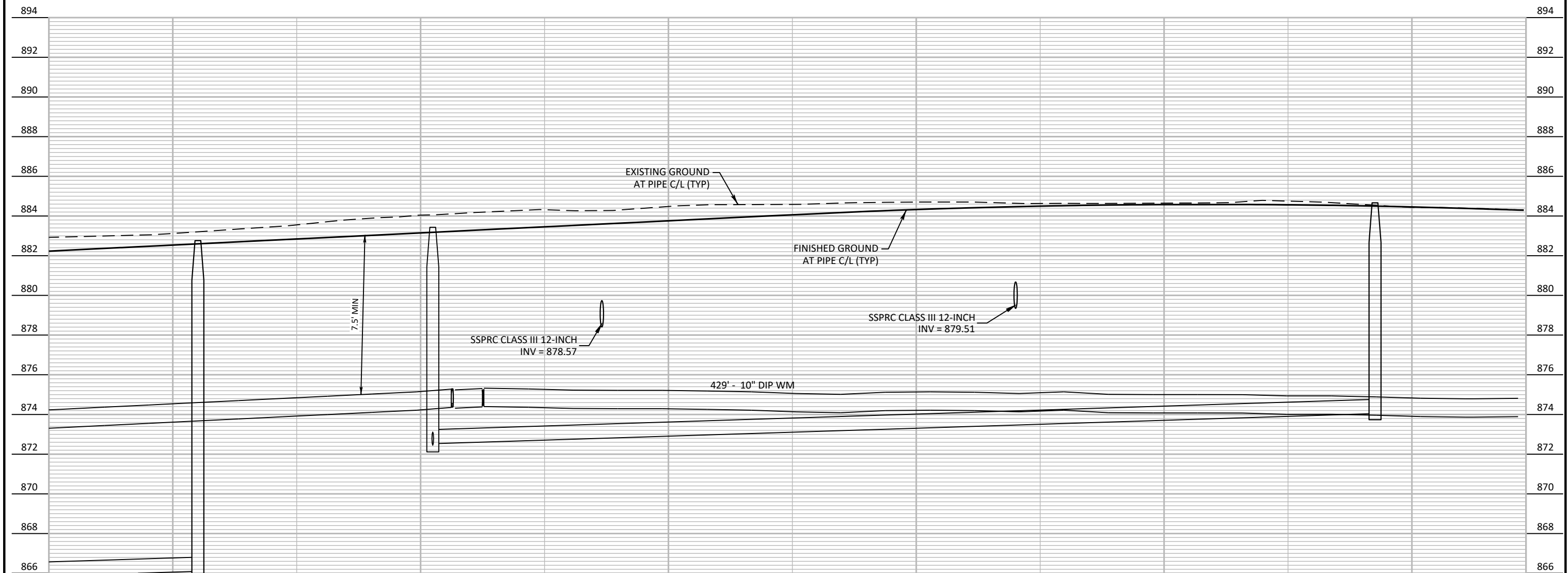
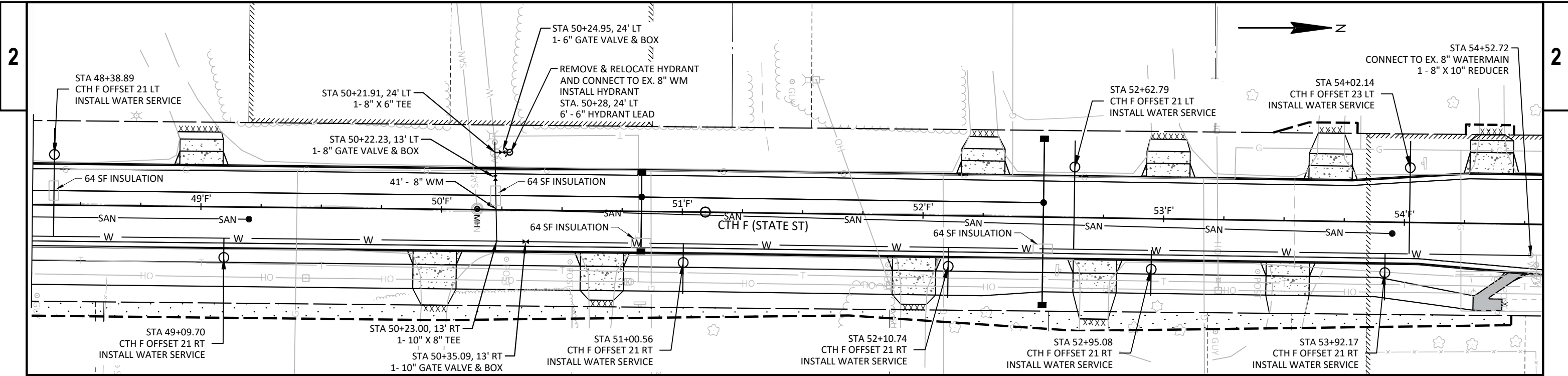


PROJECT NO: 7995-02-63	HWY: CTH F	COUNTY: EAU CLAIRE	WATER MAIN DETAILS	SHEET	E
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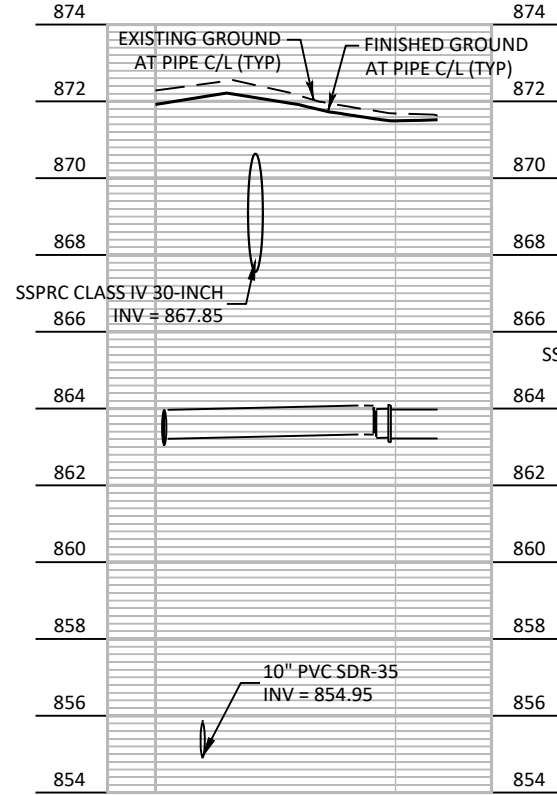
PROJECT NO: 7995-02-63	HWY: CTH F	COUNTY: EAU CLAIRE	WATER MAIN DETAILS	SHEET	E
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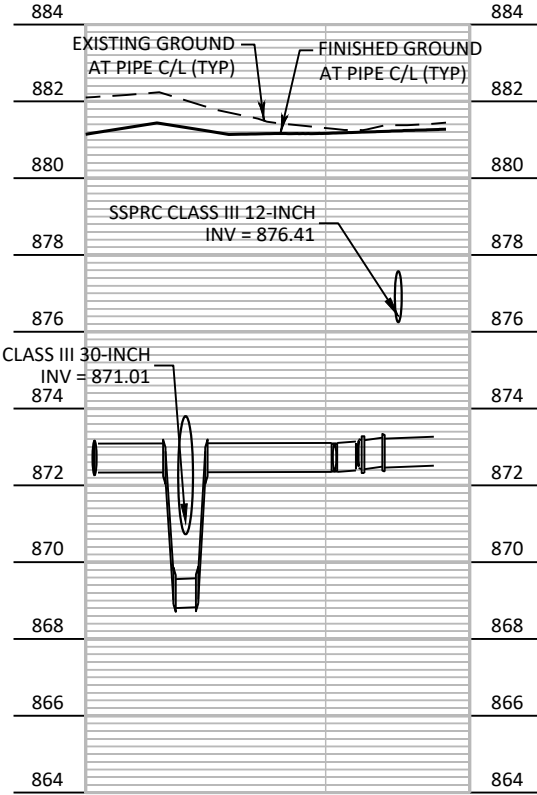


PROJECT NO: 7995-02-63	HWY: CTH F	COUNTY: EAU CLAIRE	WATER MAIN DETAILS	SHEET	E
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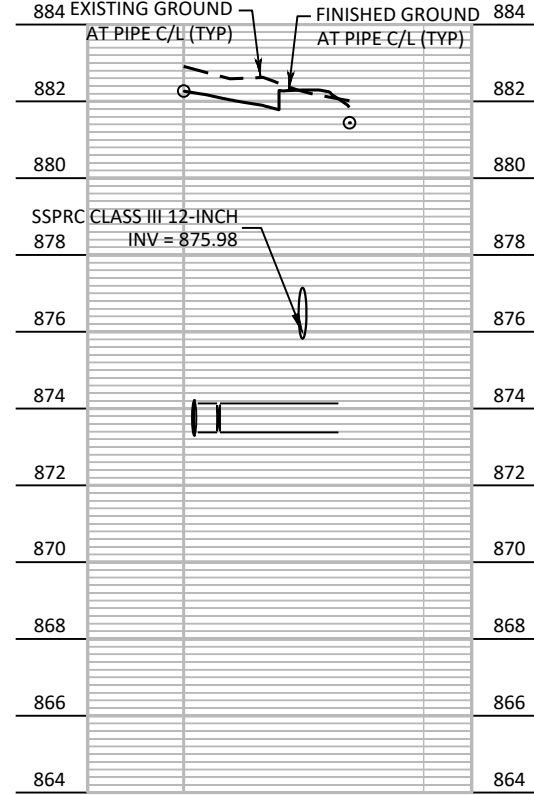
Profile View of Heather Rd wtr



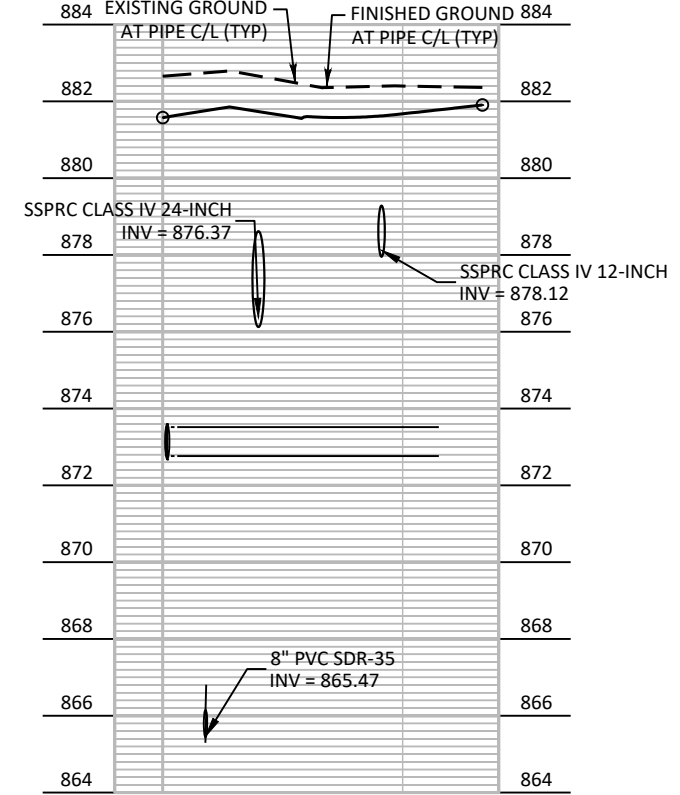
Profile View of Grover Rd wtr



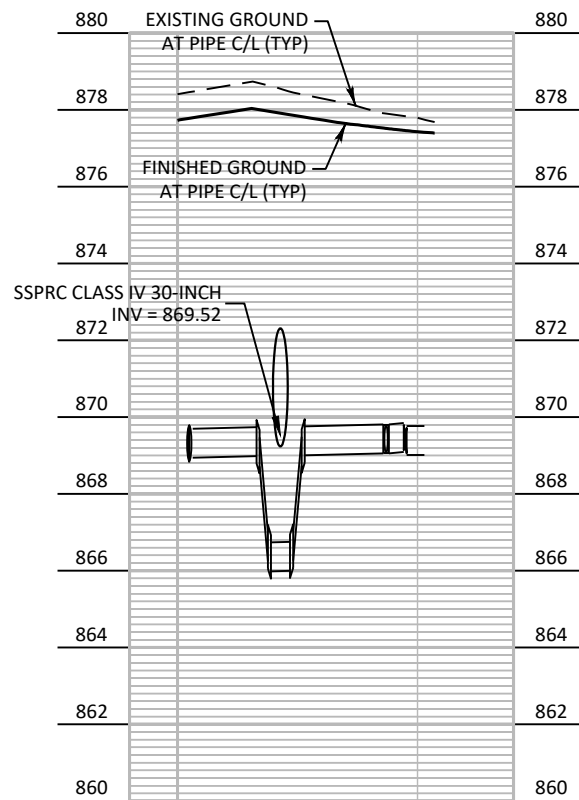
Profile View of Tamarack Ln wtr



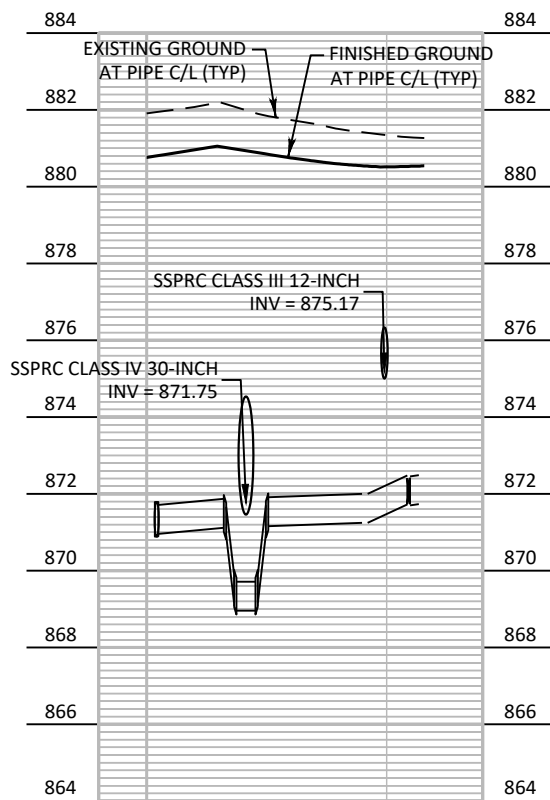
Profile View of Westover Rd wtr



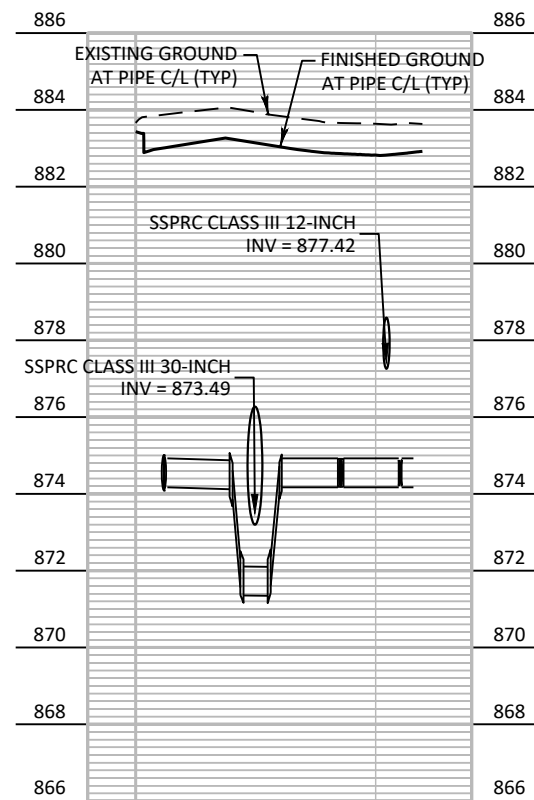
Profile View of Corydon Rd wtr



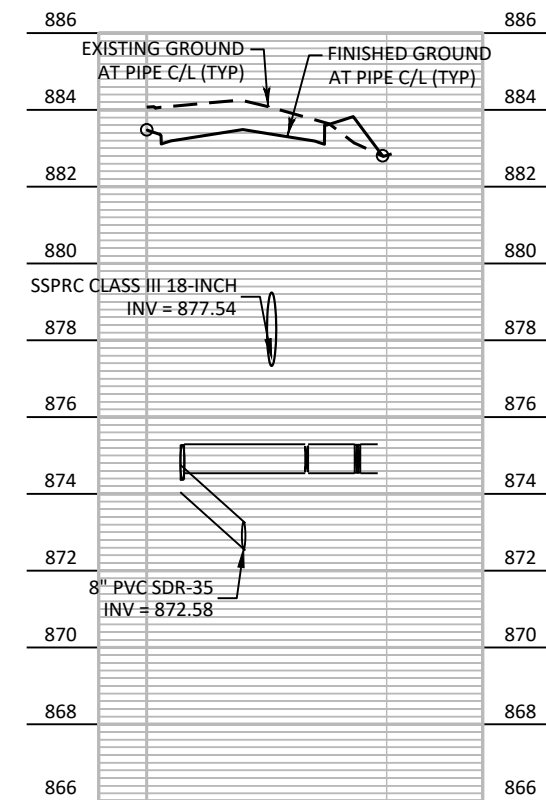
Profile View of Pine Pl S wtr

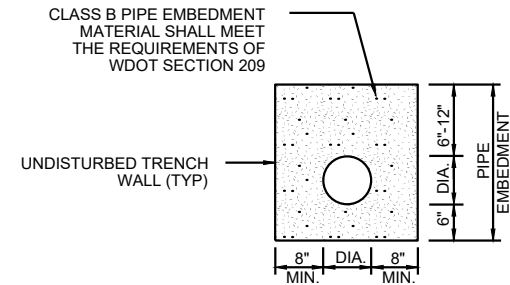


Profile View of Pine Pl N Wtr



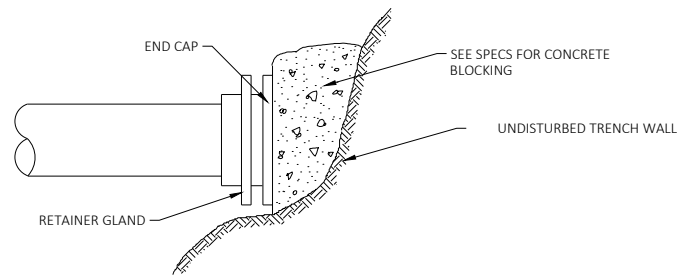
Profile View of CTH F Connection wtr



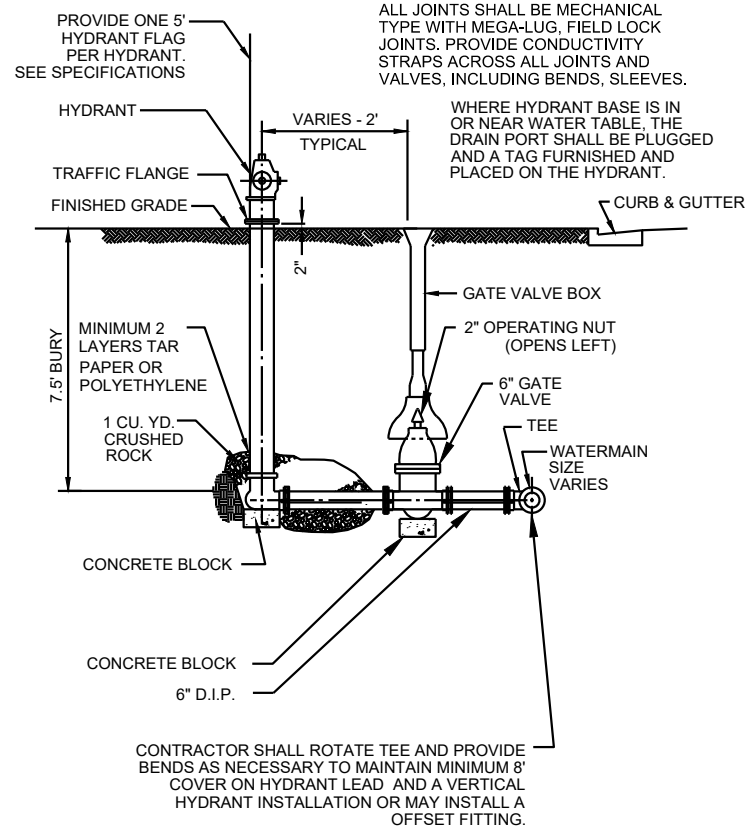


NOTE: CLASS B PIPE EMBEDMENT SHALL BE INCIDENTAL TO ALL PIPE INSTALLATION

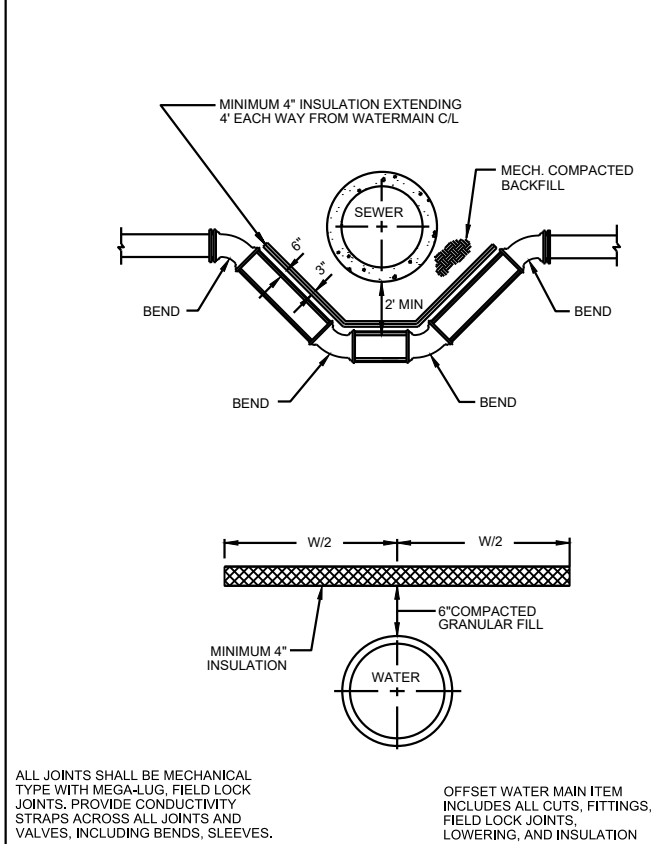
CLASS B PIPE EMBEDMENT



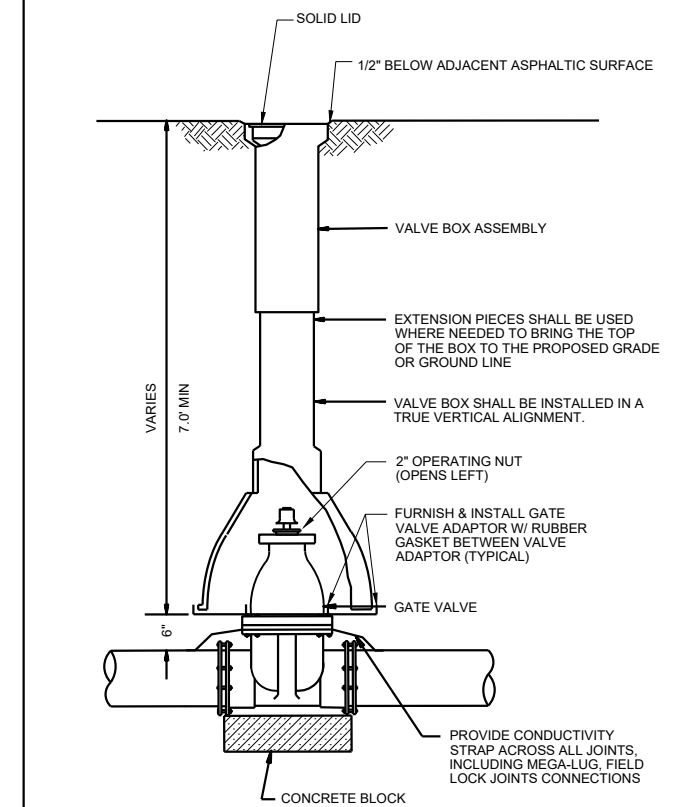
DEAD END BLOCKING



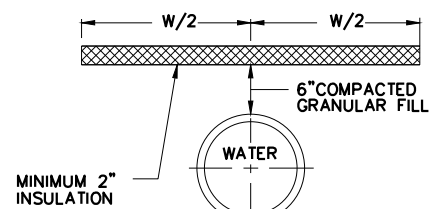
FIRE HYDRANT



OFFSET WATER MAIN



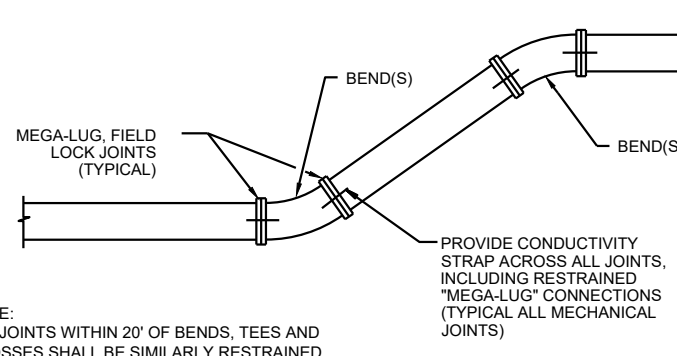
GATE VALVE & BOX



INSULATE WATER MAIN

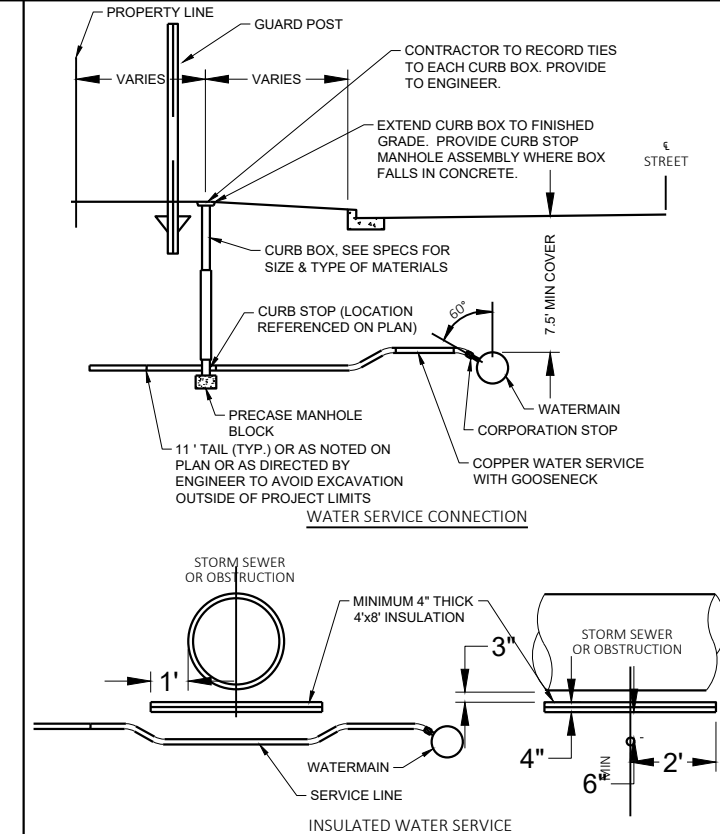
RESTRAINED/TIED PIPE LENGTH CHART								
MINIMUM LENGTH REQUIRING RESTRAINT IN FEET								
FITTING TYPE	6"	8"	10"	12"	16"	20"	24"	30"
11.25 DEGREE BEND	10	10	10	10	20	20	20	20
22.50 DEGREE BEND	10	10	10	10	20	20	40	40
45 DEGREE BEND	10	10	10	20	40	40	40	60
60 DEGREE BEND	10	20	20	30	40	60	60	80
90 DEGREE BEND	20	20	30	40	60	80	80	120
TEE (SIDE TIED)	20	20	30	40	60	80	80	120
STUBS OR DEAD ENDS	20	20	30	40	60	80	80	120
CROSS WITH PLUGGED END	20	20	30	40	60	80	80	120
VALVES AT END OF LINE	20	20	30	40	60	80	80	120

THE ABOVE LENGTHS (GIVEN IN FEET) REPRESENT THE MINIMUM LENGTH OF PIPE TO BE TIED TOGETHER IN EACH REQUIRED DIRECTION FROM THE FITTING DESCRIBED.



NOTE: ALL JOINTS WITHIN 20' OF BENDS, TEES AND CROSSES SHALL BE SIMILARLY RESTRAINED.

JOINT RESTRAINT DETAIL

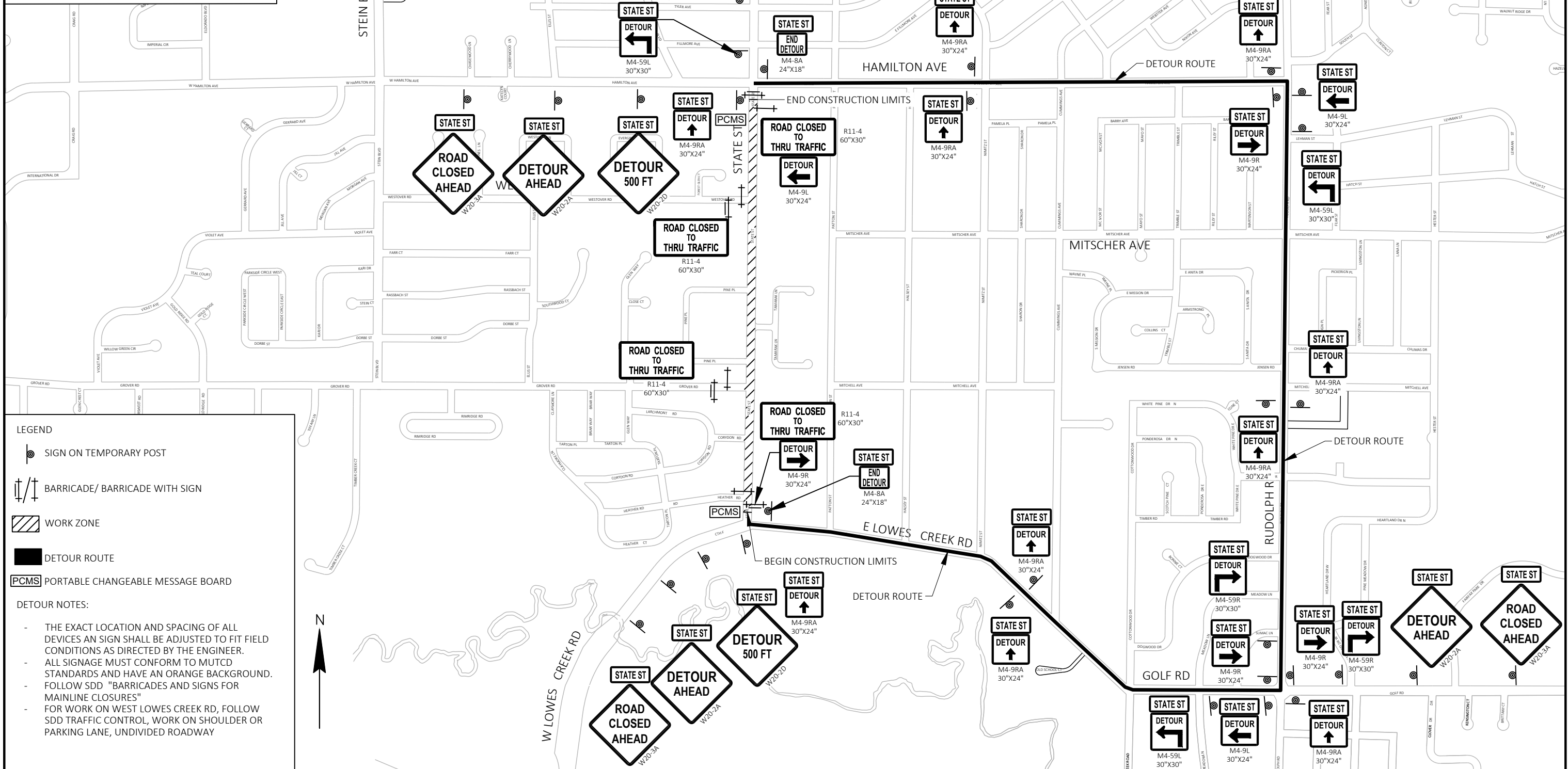


WATER SERVICE CONNECTION

NOTES:

- 1. PORTABLE CHANGEABLE MESSAGE SIGNS (PCMS) SHALL BE PLACED SEVEN DAYS PRIOR TO THE BEGINNING OF CONSTRUCTION.
- 2. COORDINATE THE APPROVAL OF PCMS MESSAGE WITH WISDOT.

PCMS MESSAGE	
FRAME 1	FRAME 2
STATE ST CLOSED	STARTING XX/XX/XX



**LEGEND**

- SIGN ON TEMPORARY POST
- BARRICADE/ BARRICADE WITH SIGN
- WORK ZONE
- DETOUR ROUTE
- PCMS PORTABLE CHANGEABLE MESSAGE BOARD

**DETOUR NOTES:**

- THE EXACT LOCATION AND SPACING OF ALL DEVICES AND SIGN SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.
- ALL SIGNAGE MUST CONFORM TO MUTCD STANDARDS AND HAVE AN ORANGE BACKGROUND. FOLLOW SDD "BARRICADES AND SIGNS FOR MAINLINE CLOSURES"
- FOR WORK ON WEST LOWES CREEK RD, FOLLOW SDD TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY



**CTH F ALIGNMENT DATA**

PI STATION	NORTHING	EASTING	DISTANCE	DIRECTION
19+17.02	265432.853	340117.849		
			354.86	N1° 49' 43.89"E
22+71.88	265787.531	340129.174		
			637.27	N1° 32' 12.48"E
29+09.15	266424.574	340146.265		
			949.93	N0° 39' 25.44"E
38+59.08	267374.441	340157.158		
			650.11	N0° 20' 35.56"E
45+09.19	268024.542	340161.053		
			600.35	N0° 37' 30.83"E
51+09.55	268624.857	340167.604		
			390.45	N0° 50' 15.35"E
55+00.00	269015.270	340173.312		

**MULTI USE PATH ALIGNMENT DATA\***

PI STATION	NORTHING	EASTING	DISTANCE	DIRECTION
1000+00.00	265641.784	340147.532		
			145.15	N1° 49' 43.89"E
1001+45.15	265786.858	340152.164		
			428.18	N1° 32' 12.48"E
1005+73.32	266214.880	340163.647		
			67.52	N1° 51' 34.64"W
1006+40.84	266282.362	340161.456		
			141.90	N1° 32' 12.48"E
1007+82.74	266424.210	340165.262		
			231.16	N0° 39' 25.44"E
1010+13.90	266655.351	340167.913		
			59.97	N4° 28' 54.06"E
1010+73.87	266715.139	340172.599		
			423.55	N0° 39' 25.44"E
1014+97.42	267138.663	340177.456		
			49.00	N0° 39' 25.44"E
1015+46.42	267187.659	340178.018		
			186.62	N0° 39' 25.44"E
1017+33.04	267374.264	340180.158		
			340.96	N0° 20' 35.56"E
1020+73.99	267715.215	340182.201		
			50.02	N1° 22' 31.91"W
1021+24.02	267765.223	340181.000		
			125.00	N0° 20' 36.11"E
1022+49.02	267890.221	340181.749		
			50.02	N2° 03' 41.65"E
1022+99.04	267940.211	340183.548		
			84.16	N0° 20' 35.56"E
1023+83.20	268024.371	340184.052		
			600.25	N0° 37' 30.83"E
1029+83.45	268624.583	340190.602		
			280.12	N0° 50' 15.35"E
1032+63.57	268904.674	340194.697		
			39.92	N10° 57' 18.12"E
1033+03.48	268943.862	340202.283		
			0.58	N1° 08' 06.31"E
1033+04.06	268944.439	340202.294		

\*LOCATED ALONG INSIDE EDGE OF PATH

PROJECT NO: 7995-02-63

HWY: CTH F

COUNTY: EAU CLAIRE

ALIGNMENT

SHEET

E

SIDE ROAD ALIGNMENT DATA

LOWES CREEK ROAD

Curve Point Data

Description	Station	Northing	Easting
PC:	0+00.000	265113.18	339417.28
RP:		264562.33	339962.43
PT:	3+35.381	265291.02	339698.54

Circular Curve Data

Parameter	Value	Parameter	Value
Delta:	24° 47' 41.0919"	Type:	RIGHT
Radius:	775.00		
Length:	335.38	Tangent:	170.36
Mid-Ord:	18.07	External:	18.50
Chord:	332.77	Course:	N 57° 41' 44.3500" E

Tangent Data

Description	PT Station	Northing	Easting
Start:	3+35.381	265291.02	339698.54
End:	4+82.228	265341.02	339836.62

Tangent Data

Parameter	Value	Parameter	Value
Length:	146.85	Course:	N 70° 05' 34.8960" E

Tangent Data

Description	PT Station	Northing	Easting
Start:	4+82.228	265341.02	339836.62
End:	7+51.451	265426.65	340091.86

Tangent Data

Parameter	Value	Parameter	Value
Length:	269.22	Course:	N 71° 27' 20.9599" E

Curve Point Data

Description	Station	Northing	Easting
PC:	7+51.451	265426.65	340091.86
RP:		265284.43	340139.57
PT:	8+12.335	265433.93	340151.89

Circular Curve Data

Parameter	Value	Parameter	Value
Delta:	23° 15' 21.6076"	Type:	RIGHT
Radius:	150.00		
Length:	60.88	Tangent:	30.87
Mid-Ord:	3.08	External:	3.14
Chord:	60.47	Course:	N 83° 05' 01.7637" E

Tangent Data

Description	PT Station	Northing	Easting
Start:	8+12.335	265433.93	340151.89
End:	9+00.000	265426.73	340239.26

Tangent Data

Parameter	Value	Parameter	Value
Length:	87.67	Course:	S 85° 17' 17.4325" E

HEATHER ROAD

PI STATION	NORTHING	EASTING	DISTANCE	DIRECTION
100+00.00	265690.337	340055.484		
			70.78	N 87° 14' 25.31" E
100+70.78	265693.744	340126.179		

CORYDON ROAD

PI STATION	NORTHING	EASTING	DISTANCE	DIRECTION
200+00.00	266191.633	340060.635		
			79.35	S 88° 52' 05.57" E
200+79.35	266190.066	340139.973		

GROVER ROAD

PI STATION	NORTHING	EASTING	DISTANCE	DIRECTION
300+00.00	266633.211	340051.880		
			96.77	S 89° 02' 02.82" E
300+96.77	266631.580	340148.639		

PINE PLACE (S)

PI STATION	NORTHING	EASTING	DISTANCE	DIRECTION
400+00.00	266865.282	340018.416		
			132.90	S 88° 44' 08.68" E
401+32.90	266862.350	340151.286		

TAMARACK LANE

PI STATION	NORTHING	EASTING	DISTANCE	DIRECTION
500+00.00	267163.264	340154.737		
			100.00	S 89° 32' 53.69" E
501+00.00	267162.475	340254.733		

PINE PLACE (N)

PI STATION	NORTHING	EASTING	DISTANCE	DIRECTION
600+00.00	267485.924	340032.825		
			125.00	S 89° 52' 07.18" E
601+25.00	267485.637	340157.825		

WESTOVER ROAD

PI STATION	NORTHING	EASTING	DISTANCE	DIRECTION
700+00.00	268273.287	340013.760		
			150.00	S 89° 38' 06.34" E
701+50.00	268272.332	340163.757		

Estimate Of Quantities

7995-02-63

Line	Item	Item Description	Unit	Total	Qty
0002	201.0205	Grubbing	STA	15.000	15.000
0004	204.0150	Removing Curb & Gutter	LF	174.000	174.000
0006	204.0155	Removing Concrete Sidewalk	SY	14.000	14.000
0008	204.0210	Removing Manholes	EACH	1.000	1.000
0010	204.0220	Removing Inlets	EACH	1.000	1.000
0012	204.0245	Removing Storm Sewer (size) 01. 12-Inch	LF	47.000	47.000
0014	204.0280	Sealing Pipes	EACH	1.000	1.000
0016	205.0100	Excavation Common	CY	14,055.000	14,055.000
0018	211.0101	Prepare Foundation for Asphaltic Paving (project) 01. 7995-02-63	EACH	1.000	1.000
0020	213.0100	Finishing Roadway (project) 01. 7995-02-63	EACH	1.000	1.000
0022	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	16,043.000	16,043.000
0024	450.4000	HMA Cold Weather Paving	TON	1,270.000	1,270.000
0026	455.0605	Tack Coat	GAL	775.000	775.000
0028	460.2000	Incentive Density HMA Pavement	DOL	3,250.000	3,250.000
0030	460.6224	HMA Pavement 4 MT 58-28 S	TON	3,619.000	3,619.000
0032	460.6244	HMA Pavement 4 MT 58-34 S	TON	1,448.000	1,448.000
0034	465.0105	Asphaltic Surface	TON	593.000	593.000
0036	465.0120	Asphaltic Surface Driveways and Field Entrances	TON	51.000	51.000
0038	522.1012	Apron Endwalls for Culvert Pipe Reinforced Concrete 12-Inch	EACH	1.000	1.000
0040	522.1030	Apron Endwalls for Culvert Pipe Reinforced Concrete 30-Inch	EACH	1.000	1.000
0042	601.0411	Concrete Curb & Gutter 30-Inch Type D	LF	6,850.000	6,850.000
0044	602.0410	Concrete Sidewalk 5-Inch	SF	755.000	755.000
0046	602.0505	Curb Ramp Detectable Warning Field Yellow	SF	72.000	72.000
0048	602.0810	Concrete Driveway 6-Inch	SY	682.000	682.000
0050	606.0200	Riprap Medium	CY	5.000	5.000
0052	608.0312	Storm Sewer Pipe Reinforced Concrete Class III 12-Inch	LF	747.000	747.000
0054	608.0318	Storm Sewer Pipe Reinforced Concrete Class III 18-Inch	LF	444.000	444.000
0056	608.0330	Storm Sewer Pipe Reinforced Concrete Class III 30-Inch	LF	1,556.000	1,556.000
0058	608.0412	Storm Sewer Pipe Reinforced Concrete Class IV 12-Inch	LF	229.000	229.000
0060	608.0415	Storm Sewer Pipe Reinforced Concrete Class IV 15-Inch	LF	34.000	34.000
0062	608.0424	Storm Sewer Pipe Reinforced Concrete Class IV 24-Inch	LF	184.000	184.000
0064	608.0430	Storm Sewer Pipe Reinforced Concrete Class IV 30-Inch	LF	1,212.000	1,212.000
0066	611.0530	Manhole Covers Type J	EACH	15.000	15.000
0068	611.0545	Manhole Covers Type L	EACH	2.000	2.000
0070	611.0624	Inlet Covers Type H	EACH	23.000	23.000
0072	611.0639	Inlet Covers Type H-S	EACH	14.000	14.000
0074	611.0642	Inlet Covers Type MS	EACH	9.000	9.000
0076	611.2004	Manholes 4-FT Diameter	EACH	2.000	2.000
0078	611.2005	Manholes 5-FT Diameter	EACH	14.000	14.000
0080	611.3004	Inlets 4-FT Diameter	EACH	13.000	13.000
0082	611.3230	Inlets 2x3-FT	EACH	25.000	25.000
0084	611.3901	Inlets Median 1 Grate	EACH	9.000	9.000
0086	618.0100	Maintenance and Repair of Haul Roads (project) 01. 7995-02-63	EACH	1.000	1.000
0088	619.1000	Mobilization	EACH	1.000	1.000
0090	623.0200	Dust Control Surface Treatment	SY	14,950.000	14,950.000
0092	624.0100	Water	MGAL	160.400	160.400
0094	625.0500	Salvaged Topsoil	SY	16,695.000	16,695.000
0096	627.0200	Mulching	SY	16,695.000	16,695.000
0098	628.1504	Silt Fence	LF	1,844.000	1,844.000
0100	628.1520	Silt Fence Maintenance	LF	1,844.000	1,844.000

Estimate Of Quantities

7995-02-63

Line	Item	Item Description	Unit	Total	Qty
0102	628.1905	Mobilizations Erosion Control	EACH	6.000	6.000
0104	628.1910	Mobilizations Emergency Erosion Control	EACH	6.000	6.000
0106	628.2004	Erosion Mat Class I Type B	SY	1,461.000	1,461.000
0108	628.7005	Inlet Protection Type A	EACH	46.000	46.000
0110	628.7015	Inlet Protection Type C	EACH	37.000	37.000
0112	628.7504	Temporary Ditch Checks	LF	50.000	50.000
0114	628.7560	Tracking Pads	EACH	2.000	2.000
0116	629.0210	Fertilizer Type B	CWT	1.170	1.170
0118	630.0140	Seeding Mixture No. 40	LB	50.000	50.000
0120	630.0200	Seeding Temporary	LB	50.000	50.000
0122	630.0500	Seed Water	MGAL	469.000	469.000
0124	633.5200	Markers Culvert End	EACH	2.000	2.000
0126	634.0614	Posts Wood 4x6-Inch X 14-FT	EACH	13.000	13.000
0128	637.2210	Signs Type II Reflective H	SF	67.760	67.760
0130	637.2230	Signs Type II Reflective F	SF	18.500	18.500
0132	638.2102	Moving Signs Type II	EACH	12.000	12.000
0134	638.2602	Removing Signs Type II	EACH	18.000	18.000
0136	638.3000	Removing Small Sign Supports	EACH	13.000	13.000
0138	642.5001	Field Office Type B	EACH	1.000	1.000
0140	643.0300	Traffic Control Drums	DAY	75.000	75.000
0142	643.0420	Traffic Control Barricades Type III	DAY	3,680.000	3,680.000
0144	643.0705	Traffic Control Warning Lights Type A	DAY	5,520.000	5,520.000
0146	643.0900	Traffic Control Signs	DAY	13,273.000	13,273.000
0148	643.1050	Traffic Control Signs PCMS	DAY	14.000	14.000
0150	643.5000	Traffic Control	EACH	1.000	1.000
0152	645.0120	Geotextile Type HR	SY	15.000	15.000
0154	646.1020	Marking Line Epoxy 4-Inch	LF	13,998.000	13,998.000
0156	646.7420	Marking Crosswalk Epoxy Transverse Line 6-Inch	LF	70.000	70.000
0158	646.7520	Marking Crosswalk Epoxy Block Style 24-Inch	LF	70.000	70.000
0160	650.4000	Construction Staking Storm Sewer	EACH	65.000	65.000
0162	650.4500	Construction Staking Subgrade	LF	3,863.000	3,863.000
0164	650.5000	Construction Staking Base	LF	3,863.000	3,863.000
0166	650.5500	Construction Staking Curb Gutter and Curb & Gutter	LF	6,850.000	6,850.000
0168	650.9000	Construction Staking Curb Ramps	EACH	4.000	4.000
0170	650.9500	Construction Staking Sidewalk (project) 01. 7995-02-63	EACH	1.000	1.000
0172	650.9911	Construction Staking Supplemental Control (project) 01. 7995-02-63	EACH	1.000	1.000
0174	650.9920	Construction Staking Slope Stakes	LF	3,863.000	3,863.000
0176	690.0150	Sawing Asphalt	LF	773.000	773.000
0178	690.0250	Sawing Concrete	LF	54.000	54.000
0180	ASP.1T0A	On-the-Job Training Apprentice at \$5.00/HR	HRS	600.000	600.000
0182	ASP.1T0G	On-the-Job Training Graduate at \$5.00/HR	HRS	800.000	800.000
0184	SPV.0060	Special 01. Construction Staking Multi-Use Path	EACH	1.000	1.000
0186	SPV.0060	Special 02. Gate Valve and Box 6-Inch	EACH	7.000	7.000
0188	SPV.0060	Special 03. Gate Valve and Box 8-Inch	EACH	8.000	8.000
0190	SPV.0060	Special 04. Gate Valve and Box 10-Inch	EACH	9.000	9.000
0192	SPV.0060	Special 05. Hydrant	EACH	7.000	7.000
0194	SPV.0060	Special 06. Curb Stop and Box 1-Inch	EACH	43.000	43.000
0196	SPV.0060	Special 07. Corporation 1-Inch	EACH	42.000	42.000
0198	SPV.0060	Special 08. Removing Hydrants	EACH	2.000	2.000
0200	SPV.0060	Special 09. Sanitary Manhole Outside Drop Section	EACH	2.000	2.000

Estimate Of Quantities

7995-02-63

Line	Item	Item Description	Unit	Total	Qty
0202	SPV.0060	Special 10. Adjusting Manhole Covers	EACH	2.000	2.000
0204	SPV.0085	Special 01. Watermain Fittings	LB	2,425.000	2,425.000
0206	SPV.0090	Special 01. Watermain 6-Inch	LF	173.000	173.000
0208	SPV.0090	Special 02. Watermain 8-Inch	LF	415.000	415.000
0210	SPV.0090	Special 03. Watermain 10-Inch	LF	3,365.000	3,365.000
0212	SPV.0090	Special 04. Water Service Copper Type K 1-Inch	LF	1,428.000	1,428.000
0214	SPV.0090	Special 05. Sanitary Sewer PVC 8-Inch	LF	2,530.000	2,530.000
0216	SPV.0090	Special 06. Sanitary Sewer PVC 10-Inch	LF	1,103.000	1,103.000
0218	SPV.0090	Special 07. Sanitary Service PVC 4-Inch	LF	1,468.000	1,468.000
0220	SPV.0090	Special 08. Removing Existing Watermain	LF	225.000	225.000
0222	SPV.0165	Special 01. Insulation	SF	3,008.000	3,008.000
0224	SPV.0200	Special 01. Sanitary Manholes & Castings 4-FT Diameter	VF	342.800	342.800

**GRUBBING**

		201.0205 GRUBBING	
STATION	LOCATION	STA	
21+00 - 21+68 'F'	RT	1	
23+70 - 23+80 'F'	RT	1	
26+00 - 29+00 'F'	RT	3	
30+50 - 31+50 'F'	RT	1	
33+00 - 34+00 'F'	RT	1	
40+50 - 41+50 'F'	RT	1	
40+65 - 41+73 'F'	LT	1	
45+35 - 45+55 'F'	RT	1	
UNDISTRIBUTED	LT&RT	5	
ITEM TOTALS		15	

**REMOVING CURB & GUTTER**

		204.0150		
STATION	LOCATION	LF	REMARKS	
54+22 - 54+62 'F'	RT	40		
54+58 - 54+62 'F'	LT	4		
700+50 - 701+15 'WR'	RT	65	WESTOVER ROAD	
700+50 - 701+15 'WR'	LT	65	WESTOVER ROAD	
ITEM TOTAL		174		

**REMOVING CONCRETE SIDEWALK**

		204.0155	
STATION	LOCATION	SY	
57+28 - 54+41 'F'	RT	14	
ITEM TOTAL		14	

**REMOVING STORM SEWER ITEMS**

		204.0245.01					
		204.0210	204.0220	REMOVING	204.0280		
		REMOVING	REMOVING	STORM SEWER	SEALING		
		MANHOLES	INLETS	01.12-INCH	PIPES		
STATION	LOCATION	EACH	EACH	LF	EACH	REMARKS	
47+91 'F'	LT	-	1	-	-		
47+47 - 47+91 'F'	LT	-	-	47	1		
701+06 'WR'	RT	1	-	-	-	WESTOVER ROAD	
ITEM TOTALS		1	1	47	1		

NOTE: ALL ITEMS ON THIS PAGE ARE CATEGORY 0010 UNLESS OTHERWISE NOTED.

3

3

EARTHWORK SUMMARY								
		INCREMENTAL VOLUME (CY)						
FROM/TO STATION	LOCATION	205.0100 COMMON EXCAVATION (1)	SALVAGED / UNUSABLE PAVEMENT MATERIAL (3)	AVAILABLE MATERIAL (4)	UNEXPANDED FILL	EXPANDED FILL (5)	MASS ORDINATE +/- (6)	COMMENT:
		CUT (2)				FACTOR 1.25		
19+17 - 21+00 'F'	CTH F	102	57	45	0	0	45	(8)
21+00 - 40+50 'F'	CTH F	6,661	1,300	5,361	375	469	4,892	-
40+50 - 54+40 'F'	CTH F	5,171	927	4,244	315	394	3,850	-
INTERSECTION	HEATHER ROAD	100	24	76	14	18	59	(7)
INTERSECTION	CORYDON ROAD	166	33	133	4	5	128	(7)
INTERSECTION	GROVER ROAD	236	51	185	48	60	125	(7)
INTERSECTION	S PINE PLACE	202	35	167	1	1	166	(7)
INTERSECTION	TAMARAK LANE	207	38	169	30	38	132	(7)
INTERSECTION	N PINE PLACE	170	31	139	3	4	135	(7)
INTERSECTION	WESTOVER ROAD	309	61	248	2	3	246	(7)
3+00 - 7+00 'LC'	LOWES CREEK ROAD	731	0	731	0	0	731	-
PROJECT TOTALS		14,055					10,508	

**NOTES:**

- (1) COMMON EXCAVATION IS THE SUM OF THE CUT . ITEM NUMBER 205.0100
- (2) SALVAGED/UNUSABLE PAVEMENT MATERIAL IS INCLUDED IN CUT.
- (3) SALVAGED/UNUSABLE PAVEMENT MATERIAL
- (4) AVAILABLE MATERIAL = CUT - SALVAGED/UNUSABLE PAVEMENT MATERIAL
- (5) EXPANDED FILL FACTOR = 1.25
- DEPENDENT ON SELECTIONS:
- (6) THE MASS ORDINATE + OR - QTY CALCULATED FOR THE STAGE. PLUS QUANTITY INDICATES AN EXCESS OF MATERIAL WITHIN THE STAGE. MINUS INDICATES A SHORTAGE OF MATERIAL WITHIN THE STAGE.
- (7) THE CUT AND FILL VALUES WERE OBTAINED FROM A SURFACE TO SURFACE VOLUMN COMPUTATION.
- (8) EARTHWORK CALCULATED AS THE PAVEMENT REPLACEMENT AREA X 19 INCH DEPTH

**PREPARE FOUNDATION FOR ASPHALTIC PAVING**

STATION	LOCATION	211.0101 EACH
PROJECT	LT & RT	1
ITEM TOTAL		1

**FINISHING ROADWAY**

STATION	213.0100 EACH	
PROJECT	1	
ITEM TOTAL		1

NOTE: ALL ITEMS ON THIS PAGE ARE CATEGORY 0010 UNLESS OTHERWISE NOTED.

**BASE AGGREGATE DENSE**

STATION	LOCATION	305.0120 624.0100		REMARKS
		1 1/4-INCH TON	WATER MGAL	
19+34 - 21+00 'F'	LT	90	0.9	MAINLINE
21+00 - 54+62 'F'	LT & RT	9517	95.2	MAINLINE
21+26 - 54+55 'F'	RT	1291	12.9	PATH
HEATHER ROAD	LT	121	1.2	INTERSECTION
CORYDON ROAD	LT	163	1.6	INTERSECTION
GROVER ROAD	LT	250	2.5	INTERSECTION
PINE PLACE SOUTH	LT	186	1.9	INTERSECTION
TAMARACK LANE	RT	193	1.9	INTERSECTION
PINE PLACE NORTH	LT	157	1.6	INTERSECTION
WESTOVER ROAD	LT	302	3.0	INTERSECTION
DRIVEWAYS	RT	159	1.6	DRIVEWAYS
DRIVEWAYS	LT	114	1.1	DRIVEWAYS
UNDISTRIBUTED	LT & RT	3500	35	FOR ANY TEMP ACCESS NEEDS
ITEM TOTALS		16043	160.4	

**ASPHALTIC PAVEMENT ITEMS**

STATION	LOCATION	450.4000				465.0120		REMARKS
		HMA COLD WEATHER PAVING TON	455.0605 TACK COAT GAL	460.6224 HMA PAVEMENT 4 MT 58-28 S TON	460.6244 HMA PAVEMENT 4 MT 58-34 S TON	465.0105 ASPHALTIC SURFACE TON	465.0120 ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES TON	
19+34 - 21+00 'F'	LT	-	-	-	-	120	-	MAINLINE
21+00 - 54+62 'F'	LT & RT	1103	675	3149	1260	-	-	MAINLINE
21+23 - 54+28 'F'	RT	-	-	-	-	473	-	PATH
HEATHER ROAD	LT	14	8	39	15	-	-	INTERSECTION
CORYDON ROAD	LT	20	12	56	22	-	-	INTERSECTION
GROVER ROAD	LT	30	18	84	34	-	-	INTERSECTION
PINE PLACE SOUTH	LT	24	14	66	27	-	-	INTERSECTION
TAMARACK LANE	RT	25	15	71	29	-	-	INTERSECTION
PINE PLACE NORTH	LT	20	12	57	23	-	-	INTERSECTION
WESTOVER ROAD	LT	34	21	96	39	-	-	INTERSECTION
DRIVEWAYS	RT	-	-	-	-	-	33	DRIVEWAYS
DRIVEWAYS	LT	-	-	-	-	-	18	DRIVEWAYS
ITEM TOTALS		1270	775	3619	1448	593	51	

**CONCRETE CURB & GUTTER 30-INCH TYPE D**

STATION	LOCATION	601.0411 LF
21+09 - 54+40 'F'	RT	3269
21+00 - 54+62 'F'	LT	2846
HEATHER ROAD	LT & RT	78
CORYDON ROAD	LT & RT	86
GROVER ROAD	LT & RT	134
PINE PLACE SOUTH	LT & RT	79
TAMARACK LANE	LT & RT	87
PINE PLACE NORTH	LT & RT	78
WESTOVER ROAD	LT & RT	193
ITEM TOTAL		6850

**SIDEWALK ITEMS**

STATION	LOCATION	602.0410 602.0505	
		CONCRETE 5-INCH SF	DETECTABLE WARNING FIELD YELLOW SF
21+30 'F'	LT & RT	216	26
36+15 'F'	RT	201	23
36+80 'F'	RT	201	23
54+34 'F'	RT	137	-
ITEM TOTALS		755	72

**CONCRETE DRIVEWAY**

STATION	LOCATION	602.0810 CONCRETE DRIVEWAY 6-INCH SY
DRIVEWAYS	LT	206
DRIVEWAYS	RT	476
ITEM TOTAL		682

NOTE: ALL ITEMS ON THIS PAGE ARE CATEGORY 0010 UNLESS OTHERWISE NOTED.



3

3

STORM SEWER PIPES (1 OF 2)

FROM	TO	608.0312	608.0318	608.0330	608.0412	608.0415	608.0424	608.0430	**JOINT TIES EACH	INLET ELEVATION	DISCHARGE ELEVATION	SLOPE FT/FT
		STORM SEWER PIPE REINFORCED CONCRETE CLASS III	STORM SEWER PIPE REINFORCED CONCRETE CLASS III	STORM SEWER PIPE REINFORCED CONCRETE CLASS III 30-INCH	STORM SEWER PIPE REINFORCED CONCRETE CLASS IV	STORM SEWER PIPE REINFORCED CONCRETE CLASS IV	STORM SEWER PIPE REINFORCED CONCRETE CLASS IV	STORM SEWER PIPE REINFORCED CONCRETE CLASS				
		12-INCH LF	18-INCH LF	LF	12-INCH LF	15-INCH LF	24-INCH LF	IV 30-INCH LF				
AEW-1	MH-2	-	-	-	-	-	-	84	6	867.18	866.96	0.0030
MH-2	MH-3	-	-	-	-	-	-	270	-	867.97	867.29	0.0030
INL-3A	MH-3	-	-	-	11	-	-	-	-	869.63	869.60	0.0050
INL-3B	MH-3	-	-	-	23	-	-	-	-	869.69	869.60	0.0050
MH-4	MH-3	-	-	-	-	-	-	205	-	868.68	868.09	0.0030
INL-4A	MH-4	-	-	-	11	-	-	-	-	870.23	870.20	0.0050
INL-4B	MH-4	-	-	-	23	-	-	-	-	870.28	870.20	0.0050
INL-4C	INL-4B	30	-	-	-	-	-	-	-	870.53	870.40	0.0050
MH-5	MH-4	-	-	-	-	-	-	301	-	869.68	868.80	0.0030
INL-5A	MH-5	11	-	-	-	-	-	-	-	873.63	873.50	0.0200
INL-5B	MH-5	23	-	-	-	-	-	-	-	873.86	873.50	0.0200
MH-6	MH-5	-	-	327	-	-	-	-	-	870.76	869.80	0.0030
INL-6A	MH-6	11	-	-	-	-	-	-	-	874.47	874.37	0.0200
INL-6B	INL-6A	53	-	-	-	-	-	-	-	875.61	874.67	0.0200
INL-6C	INL-6B	36	-	-	-	-	-	-	-	876.45	875.81	0.0200
MH-7	MH-6	-	-	196	-	-	-	-	-	871.35	870.78	0.0030
INL-7A	MH-7	11	-	-	-	-	-	-	-	875.57	875.45	0.0190
INL-7B	MH-7	23	-	-	-	-	-	-	-	875.80	875.45	0.0200
MH-8	MH-7	-	-	65	-	-	-	-	-	871.55	871.37	0.0030
INL-8A	MH-8	11	-	-	-	-	-	-	-	873.90	873.80	0.0190
INL-8B	MH-8	23	-	-	-	-	-	-	-	874.16	873.80	0.0200
INL-8C	INL-8A	28	-	-	-	-	-	-	-	874.56	874.10	0.0200
INL-8D	INL-8C	30	-	-	-	-	-	-	-	875.24	874.26	0.0190
MH-9	MH-8	-	-	80	-	-	-	-	-	871.84	871.62	0.0030
INL-9A	MH-9	11	-	-	-	-	-	-	-	875.61	875.49	0.0190
MH-10	MH-9	-	-	303	-	-	-	-	-	872.70	871.96	0.0030
INL-10A	MH-10	11	-	-	-	-	-	-	-	875.51	875.39	0.0190
INL-10B	MH-10	23	-	-	-	-	-	-	-	875.54	875.36	0.0100
INL-10C	INL-10B	37	-	-	-	-	-	-	-	876.01	875.68	0.0100
INL-10E	INL-10C	16	-	-	-	-	-	-	-	876.41	876.18	0.0200
MH-11	MH-10	-	-	316	-	-	-	-	-	873.59	872.82	0.0030
INL-11A	MH-11	11	-	-	-	-	-	-	-	876.16	876.06	0.0190
INL-11B	INL-11A	29	-	-	-	-	-	-	-	876.84	876.36	0.0200
INL-11C	INL-11B	27	-	-	-	-	-	-	-	877.49	877.04	0.0200
MH-12	MH-11	-	-	104	-	-	-	-	-	873.95	873.71	0.0030
INL-12A	MH-12	25	-	-	-	-	-	-	-	875.92	875.52	0.0200
AEW-12B	MH-12	40	-	-	-	-	-	-	6	880.76	878.60	0.0590
MH-13	MH-12	-	-	165	-	-	-	-	-	874.36	873.97	0.0025
INL-13A	MH-13	11	-	-	-	-	-	-	-	876.15	876.03	0.0190

PAGE 1 - SUBTOTALS 531 0 1,556 68 0 0 860 12

\*\*NON-BID ITEM: FOR INFORMATION ONLY

NOTE: ALL ITEMS ON THIS PAGE ARE CATEGORY 0010 UNLESS OTHERWISE NOTED.

STORM SEWER PIPES (2 OF 2)

FROM	TO	608.0312	608.0318	608.0330	608.0412	608.0415	608.0424	608.0430	** JOINT TIES EACH	INLET ELEVATION	DISCHARGE ELEVATION	SLOPE FT/FT
		STORM SEWER PIPE REINFORCED CONCRETE CLASS III 12-INCH LF	STORM SEWER PIPE REINFORCED CONCRETE CLASS III 18-INCH LF	STORM SEWER PIPE REINFORCED CONCRETE CLASS III 30-INCH LF	STORM SEWER PIPE REINFORCED CONCRETE CLASS IV 12-INCH LF	STORM SEWER PIPE REINFORCED CONCRETE CLASS IV 15-INCH LF	STORM SEWER PIPE REINFORCED CONCRETE CLASS IV 24-INCH LF	STORM SEWER PIPE REINFORCED CONCRETE CLASS IV 30-INCH LF				
INL-13B	MH-13	23	-	-	-	-	-	-	-	876.39	876.03	0.020
MH-14	MH-13	-	-	-	-	-	-	271	-	875.13	874.48	0.003
INL-14A	MH-14	11	-	-	-	-	-	-	-	876.79	876.76	0.005
INL-14B	MH-14	23	-	-	-	-	-	-	-	876.85	876.76	0.005
INL-14C	INL-14A	13	-	-	-	-	-	-	-	877.11	876.93	0.019
MH-15	MH-14	-	-	-	-	-	-	81	-	875.43	875.25	0.003
INL-15A	MH-15	-	-	-	-	11	-	-	-	876.82	876.81	0.005
INL-15B	MH-15	-	-	-	-	23	-	-	-	876.89	876.81	0.005
INL-15C	INL-15A	-	-	-	21	-	-	-	-	877.19	877.11	0.005
INL-15D	INL-15B	-	-	-	20	-	-	-	-	877.25	877.17	0.005
INL-15E	INL-15C	12	-	-	-	-	-	-	-	877.49	877.33	0.019
MH-16	MH-15	-	-	-	-	-	184	-	-	876.49	876.05	0.003
INL-16A	MH-16	-	-	-	11	-	-	-	-	877.54	877.51	0.005
INL-16B	MH-16	-	-	-	23	-	-	-	-	877.61	877.51	0.005
INL-16C	INL-16A	32	-	-	-	-	-	-	-	877.88	877.68	0.008
INL-16D	INL-16C	-	-	-	43	-	-	-	-	878.21	878.01	0.005
INL-16E	INL-16B	22	-	-	-	-	-	-	-	878.11	877.75	0.020
INL-16F	INL-16A	19	-	-	-	-	-	-	-	877.98	877.71	0.019
MH-17	MH-16	-	276	-	-	-	-	-	-	877.68	877.01	0.003
INL-17A	MH-17	11	-	-	-	-	-	-	-	878.37	878.24	0.019
INL-17B	MH-17	23	-	-	-	-	-	-	-	878.61	878.24	0.020
MH-18	MH-17	-	168	-	-	-	-	-	-	878.62	877.80	0.005
INL-18A	MH-18	27	-	-	-	-	-	-	-	879.62	879.18	0.020
INL-18B	MH-18	-	-	-	43	-	-	-	-	881.07	880.31	0.020
PAGE 2 - SUBTOTALS		216	444	0	161	34	184	352	0			
ITEM TOTALS		747	444	1,556	229	34	184	1,212	12			

\*\*NON-BID ITEM: FOR INFORMATION ONLY

NOTE: ALL ITEMS ON THIS PAGE ARE CATEGORY 0010 UNLESS OTHERWISE NOTED.



STORM SEWER STRUCTURES (2 OF 2)

522.1012 APRON  
 522.1030 APRON  
 ENDWALLS FOR ENDWALLS FOR  
 CULVERT PIPE FOR CULVERT

STRUCTURE	** STATION	** OFFSET	522.1012 APRON		522.1030 APRON		611.0530	611.0545	611.0624	611.0639	611.0642	611.2004	611.2005	611.3004	611.3230	611.3901	633.5200	650.4000	RIM ELEVATION	INVERT ELEVATION	*** DEPTH FT
			REINFORCED CONCRETE 12-INCH EACH	REINFORCED CONCRETE 30-INCH EACH	MANHOLE COVERS TYPE J EACH	MANHOLE COVERS TYPE L EACH	INLET COVERS TYPE H EACH	INLET COVERS TYPE H-S EACH	INLET COVERS TYPE MS EACH	MANHOLES 4-FT DIAMETER EACH	MANHOLES 5-FT DIAMETER EACH	INLETS 4-FT DIAMETER EACH	INLETS 2X3-FT EACH	**** INLETS MEDIAN 1 GRATE EACH	CULVERT END MARKERS EACH	CONSTRUCTION STAKING SEWER EACH					
MH-13	42+75'F	6.0'LT	-	-	1	-	-	-	-	-	-	-	1	-	-	-	-	1	882.21	874.36	6.60
INL-13A	42+75'F	16.5'LT	-	-	-	-	-	1	-	-	-	-	-	-	1	-	-	1	881.97	876.15	4.82
INL-13B	42+75'F	16.5'RT	-	-	-	-	-	1	-	-	-	-	-	-	1	-	-	1	881.97	876.39	4.58
MH-14	45+45'F	6.0'LT	-	-	1	-	-	-	-	-	-	-	1	-	-	-	-	1	881.40	875.13	5.02
INL-14A	45+45'F	16.5'LT	-	-	-	-	-	1	-	-	-	-	-	-	1	-	-	1	881.16	876.79	3.37
INL-14B	45+45'F	16.5'RT	-	-	-	-	-	1	-	-	-	-	-	-	1	-	-	1	881.16	876.85	3.06
INL-14C	45+45'F	29.0'LT	-	-	-	-	-	-	-	1	-	-	-	-	-	1	-	1	880.71	877.11	3.60
MH-15	46+25'F	6.0'LT	-	-	1	-	-	-	-	-	-	-	1	-	-	-	-	1	881.25	875.43	4.57
INL-15A	46+25'F	16.5'LT	-	-	-	-	-	-	1	-	-	-	-	-	1	-	-	1	881.01	876.82	3.19
INL-15B	46+25'F	16.5'RT	-	-	-	-	-	-	1	-	-	-	-	-	1	-	-	1	881.01	876.89	3.12
INL-15C	46+45'F	16.5'LT	-	-	-	-	-	-	1	-	-	-	-	-	1	-	-	1	881.01	877.19	2.82
INL-15D	46+45'F	16.5'RT	-	-	-	-	-	-	1	-	-	-	-	-	1	-	-	1	881.01	877.25	2.76
INL-15E	46+45'F	28.0'LT	-	-	-	-	-	-	-	1	-	-	-	-	-	1	-	1	880.85	877.49	3.36
MH-16	48+08'F	6.0'LT	-	-	1	-	-	-	-	-	-	-	-	1	-	-	-	1	882.05	876.49	4.31
INL-16A	48+08'F	16.5'LT	-	-	-	-	-	1	-	-	-	-	-	1	-	-	-	1	881.81	877.54	3.27
INL-16B	48+08'F	16.5'RT	-	-	-	-	-	1	-	-	-	-	-	-	1	-	-	1	881.81	877.61	3.20
INL-16C	47+80'F	31.1'LT	-	-	-	-	-	-	1	-	-	-	-	1	-	-	-	1	881.32	877.88	2.44
INL-16D	47+37'F	32.1'LT	-	-	-	-	-	1	-	-	-	-	-	1	-	-	-	1	881.38	878.21	2.17
INL-16E	48+08'F	38.0'RT	-	-	-	-	-	-	-	1	-	-	-	-	-	1	-	1	881.77	878.11	3.66
INL-16F	48+22'F	28.0'LT	-	-	-	-	-	-	-	1	-	-	-	-	-	1	-	1	881.32	877.98	3.34
MH-17	50+83'F	6.0'LT	-	-	1	-	-	-	-	-	1	-	-	-	-	-	-	1	883.74	877.68	4.81
INL-17A	50+83'F	16.5'LT	-	-	-	-	-	1	-	-	-	-	-	-	1	-	-	1	883.50	878.37	4.13
INL-17B	50+83'F	16.5'RT	-	-	-	-	-	1	-	-	-	-	-	-	1	-	-	1	883.50	878.61	3.89
MH-18	52+50'F	6.0'LT	-	-	1	-	-	-	-	-	1	-	-	-	-	-	-	1	884.61	878.62	4.74
INL-18A	52+50'F	32.5'LT	-	-	-	-	-	-	-	1	-	-	-	-	-	1	-	1	883.45	879.62	3.83
INL-18B	52+50'F	36.5'RT	-	-	-	-	-	-	-	1	-	-	-	-	-	1	-	1	883.90	881.07	2.83

PAGE 2 - SUBTOTALS	0	0	6	0	9	5	6	2	3	4	11	6	0	26
ITEM TOTALS	1	1	15	2	23	14	9	2	14	13	25	9	2	65

\*ADDITIONAL QUANTITIES LISTED ELSEWHERE IN PLANS  
 \*\* STATION & OFFSET ARE TO CENTER OF STRUCTURE  
 \*\*\* DEPTH = RIM ELEV - TOP OF STR ELEV - COVER HT - 6 INCH ADJ RING HT (MEDIAN INLET DEPTH INCLUDES COVER HT AND NO ADJ RING HT)  
 \*\*\*\*NO SLOPE

NOTE: ALL ITEMS ON THIS PAGE ARE CATEGORY 0010 UNLESS OTHERWISE NOTED.

**MOBILIZATION**

STATION	619.1000 EACH
PROJECT	1
<b>ITEM TOTAL</b>	<b>1</b>

**FINISHING ITEMS**

STATION	LOCATION	625.0500	627.0200	629.0210	630.0140	630.0200	630.0500
		SALVAGED TOPSOIL SY	MULCHING SY	FERTILIZER TYPE B CWT	SEEDING MIXTURE NO. 40 LB	SEEDING TEMPORARY LB	SEED WATER MGAL
21+00 - 54+62 'F'	RT	3244	3244	0.23	10	10	91
21+00 - 54+62 'F'	LT	11933	11933	0.84	36	36	335
UNDISTRIBUTED (10%)		1518	1518	0.11	5	5	43
<b>ITEM TOTALS</b>		<b>16695</b>	<b>16695</b>	<b>1.17</b>	<b>50</b>	<b>50</b>	<b>469</b>

**EROSION CONTROL ITEMS**

STATION	LOCATION	606.0200	645.0120	623.0200	628.1504	628.1520	628.2004	628.7005	628.7015	628.7504	628.7560
		RIPRAP MEDIUM CY	GEOTEXTILE TYPE HR SY	DUST CONTROL SURFACE TREATMENT SY	SILT FENCE SILT FENCE LF	SILT FENCE MAINTENANCE LF	EROSION MAT CLASSI TYPE B SY	INLET PROTECTION TYPE A EACH	INLET PROTECTION TYPE C EACH	TEMPORARY DITCH CHECKS LF	EROSION TRACKING PADS EACH
21+00 - 54+62 'F'	RT	-	-	-	922	922	-	-	-	-	-
21+00 - 54+62 'F'	LT	-	-	-	553	553	-	-	-	-	-
21+00 - 54+62 'F'	LT/RT	-	-	14950	-	-	-	46	37	-	2
W LOWES CREEK RD	LT	5	15	-	-	-	1169	-	-	40	-
UNDISTRIBUTED (25%)		-	-	-	369	369	292	-	-	10	-
<b>ITEM TOTALS</b>		<b>5</b>	<b>15</b>	<b>14950</b>	<b>1844</b>	<b>1844</b>	<b>1461</b>	<b>46</b>	<b>37</b>	<b>50</b>	<b>2</b>

**MOBILIZATIONS EROSION CONTROL**

STATION	628.1905	628.1910
	EROSION CONTROL EACH	EMERGENCY EROSION CONTROL EACH
21+00 - 54+62 'F'	6	6
<b>ITEM TOTALS</b>	<b>6</b>	<b>6</b>

NOTE: ALL ITEMS ON THIS PAGE ARE CATEGORY 0010 UNLESS OTHERWISE NOTED.

**FIELD OFFICE TYPE B**

STATION	642.5001
EACH	
PROJECT	1
ITEM TOTAL	1

**PERMANENT SIGNING ITEMS**

SIGN NUMBER	LOCATION	SIGN CODE	MESSAGE	TYPE II SIZE	637.2210	637.2230	634.0614	638.2102	638.2602	638.3000	REMARKS
					REFLECTIVE H SF	REFLECTIVE F SF	POSTS WOOD 4X6-INCH 14-FT EACH	SIGNS MOVING TYPE II EACH	SIGNS REMOVING TYPE II EACH	REMOVING SMALL SIGN SUPPORTS EACH	
1-1	LT	J12-1	CTH S / CTH F LT ARROW / AHEAD RT ARROW	48" X 45"	15.00	-	1	-	4	1	REPLACE
1-2	LT	R1-1	STOP	30" X 30"	5.18	-	1	-	1	1	REPLACE, PLACE AS CLOSE TO ROADWAY AS POSSIBLE
1-3	LT	J1-1	JCT / CTH S	24" X 39"	6.50	-	1	-	2	1	REPLACE
1-4	RT	R2-1	SPEED LIMIT 30	24" X 30"	5.00	-	1	-	1	1	REPLACE
2-1	LT	R1-1	STOP	30" X 30"	5.18	-	1	-	1	1	REPLACE, PLACE AS CLOSE TO ROADWAY AS POSSIBLE
2-2	LT	R1-1	STOP	30" X 30"	5.18	-	1	-	1	1	REPLACE, PLACE AS CLOSE TO ROADWAY AS POSSIBLE
2-3	RT	W1-7	NIGHT ARROW (DOUBLE)	48" X 24"	-	8.0	1	-	1	1	REPLACE
3-1	RT	R1-2	STOP	30" X 30"	5.18	-	1	-	1	1	REPLACE, PLACE AS CLOSE TO ROADWAY AS POSSIBLE
3-2	LT	R1-1	STOP	30" X 30"	5.18	-	1	-	1	1	REPLACE, PLACE AS CLOSE TO ROADWAY AS POSSIBLE
4-1	RT	R1-1	STOP	30" X 30"	5.18	-	1	-	1	1	REPLACE, PLACE AS CLOSE TO ROADWAY AS POSSIBLE
5-1	LT	R1-1	STOP	30" X 30"	5.18	-	1	-	1	1	REPLACE, PLACE AS CLOSE TO ROADWAY AS POSSIBLE
6-1	LT	R2-1	SPEED LIMIT 30	24" X 30"	5.00	-	1	-	1	1	REPLACE
6-2	RT	S1-1	PEDESTRIAN CROSSING	36" X 36"	-	6.75	1	-	1	1	REPLACE
		S16-9P	AHEAD	30" X 18"	-	3.75	-	-	1	-	REPLACE
UNDISTRIBUTED	LT & RT	-	-	-	-	-	-	12	-	-	SALVAGE & REINSTALL EXISTING SIGNS
ITEM TOTALS					67.76	18.50	13	12	18	13	

**TRAFFIC CONTROL**

STATION	LOCATION	643.0705						643.5000
		643.0030	643.0420	643.0900	643.1050	643.0900	643.1050	
		BARRICADES DRUMS DAY	WARNING TYPE III DAY	LIGHTS TYPE A DAY	SIGNS DAY	PCMS DAY	TRAFFIC CONTROL EACH	
21+00 - 54+62 'F'	CTH F	-	3680	5520	13248	14	1	
3+00 - 9+00 'LC'	W LOWES CREEK RD	75	-	-	25	-	-	
ITEM TOTALS		75	3680	5520	13273	14	1	

**MARKING LINES**

STATION	LOCATION	646.7420 646.7520			
		646.1020		CROSSWALK	CROSSWALK
		EDGE LINE 4-INCH	CENTER LINE 4-INCH	EPOXY	EPOXY
		WHITE LF	YELLOW LF	TRANSVERSE LINE 6-INCH LF	BLOCK STYLE 24-INCH LF
19+34 - 54+62 'F'	CTH F	6908	7090	70	70
ITEM TOTALS		13998		70	70

NOTE: ALL ITEMS ON THIS PAGE ARE CATEGORY 0010 UNLESS OTHERWISE NOTED.

**CONSTRUCTION STAKING ITEMS\***

STATION	LOCATION	650.5500		650.9000		650.9911		650.9920		650.9500		SPV.0060.01	
		650.4500	650.5000	CURB GUTTER	CURB	SUPPLEMENTAL	SLOPE	SIDEWALK	MULTI-USE	7995-02-63	7995-02-63	7995-02-63	7995-02-63
		SUBGRADE	BASE	AND GUTTER	RAMPS	CONTROL	STAKES	7995-02-63	7995-02-63	7995-02-63	7995-02-63	7995-02-63	7995-02-63
		LF	LF	LF	EACH	EACH	LF	EACH	EACH	LF	EACH	LF	EACH
21+00 -54+62 'F'	LT & RT	3362	3362	-	4	1	3362	1	1				
21+00 -54+62 'F'	RT	-	-	3269	-	-	-	-	-				
21+00 -54+62 'F'	LT	-	-	2846	-	-	-	-	-				
HEATHER ROAD	LT & RT	41	41	78	-	-	41	-	-				
CORYDON ROAD	LT & RT	60	60	86	-	-	60	-	-				
GROVER ROAD	LT & RT	82	82	134	-	-	82	-	-				
PINE PLACE SOUTH	LT & RT	78	78	79	-	-	78	-	-				
TAMARACK LANE	LT & RT	85	85	87	-	-	85	-	-				
PINE PLACE NORTH	LT & RT	70	70	78	-	-	70	-	-				
WESTOVER ROAD	LT & RT	85	85	193	-	-	85	-	-				
<b>ITEM TOTALS</b>		<b>3863</b>	<b>3863</b>	<b>6850</b>	<b>4</b>	<b>1</b>	<b>3863</b>	<b>1</b>	<b>1</b>				

\*ADDITIONAL CONSTRUCTION STAKING ITEMS LISTED ELSEWHERE IN THE PLANS.

**SAWING**

STATION	LOCATION	690.0150		690.0250	
		SAWING	SAWING	SAWING	SAWING
		ASPHALT	CONCRETE	ASPHALT	CONCRETE
		LF	LF	LF	LF
19+34 - 21+00 'F'	CTH F	276	-		
100+15 'HR'	HEATHER RD	21	-		
200+20 'CR'	CORYDON RD	37	-		
300+00 'GR'	GROVER RD	22	-		
400+50 'PS'	PINE PL SOUTH	31	-		
501+00 'TL'	TAMARACK LN	21	-		
600+40 'PN'	PINE PL NORTH	22	-		
700+50 'WR'	WESTOVER RD	32	4		
54+40 - 54+62 'F'	CTH F	40	10		
DRIVEWAYS	VARIOUS	271	40		
<b>ITEM TOTAL</b>		<b>773</b>	<b>54</b>		

NOTE: ALL ITEMS ON THIS PAGE ARE CATEGORY 0010 UNLESS OTHERWISE NOTED.

3

**WATERMAIN ITEMS**

BEGIN STATION	END STATION	SPV.0060.02 GATE VALVE AND BOX 6-INCH EACH	SPV.0060.03 GATE VALVE AND BOX 8-INCH EACH	SPV.0060.04 GATE VALVE AND BOX 10-INCH EACH	SPV.0060.05 HYDRANT EACH	SPV.0085.01 WATERMAIN FITTINGS LB	SPV.0090.01 WATERMAIN 6-INCH LF	SPV.0090.02 WATERMAIN 8-INCH LF	SPV.0090.03 WATERMAIN 10-INCH LF	SPV.0165.01 INSULATION SF
21+00	21+67	1	1	1	1	488	40	57	71	128
21+67	26+67	1	1	1	1	314	20	50	500	448
26+67	31+30	1	1	1	1	314	15	71	463	320
31+30	33+55	1	1	1	1	252	18	56	226	320
33+55	36+65	-	1	-	-	170	10	25	310	256
36+65	39+62	1	1	1	1	276	20	54	300	320
39+62	47+56	1	1	2	1	276	40	60	795	896
47+56	54+53	1	1	2	1	335	10	42	700	320
ITEM TOTALS		7	8	9	7	2425	173	415	3365	3008

**WATER SERVICE ITEMS**

NAME	STATION	OFFSET	SPV.0060.06 CURB STOP AND BOX 1-INCH EACH	SPV.0060.07 CORPORATION 1-INCH EACH	SPV.0090.04 WATER SERVICE COPPER TYPE K 1-INCH LF
W1	22+65.82	32'LT	1	1	45
W2	23+19.26	32'RT	1	1	20
W3	23+59.75	32'LT	1	1	45
W4	24+83.16	32'LT	1	1	45
W5	24+87.49	32'RT	1	1	20
W6	25+64.83	32'LT	1	1	45
W7	26+26.33	32'RT	1	1	20
W8	27+03.45	32'RT	1	1	20
W9	28+02.11	32'RT	1	1	20
W10	28+03.61	32'LT	1	1	45
W11	28+60.65	32'RT	1	1	20
W12	29+29.49	32'RT	1	1	20
W13	29+90.01	32'LT	1	1	45
W14	30+42.70	32'RT	1	1	45
W15	32+40.39	32'LT	1	1	45
W16	34+16.86	32'LT	1	1	45
W17	35+75.32	32'LT	1	1	45
W18	36+59.17	32'LT	1	1	45
W19	37+56.73	32'LT	2	1	45
W20	38+86.63	32'LT	1	1	45
W21	40+22.41	32'LT	1	1	45
W22	41+47.84	32'LT	1	1	45
W23	41+97.17	32'RT	1	1	20
W24	42+47.44	32'LT	1	1	45
W25	42+61.22	32'RT	1	1	20
W26	43+16.21	32'RT	1	1	20
W27	43+30.56	32'LT	1	1	45
W28	43+91.17	32'LT	1	1	45
W29	43+94.23	32'RT	1	1	25
W30	44+91.39	32'LT	1	1	45
W31	45+85.61	32'LT	1	1	45
W32	46+15.07	33'RT	1	1	21
W33	46+86.10	33'RT	1	1	21
W34	47+72.22	33'RT	1	1	21
W35	48+38.89	32'LT	1	1	45
W36	49+09.70	33'RT	1	1	21
W37	51+00.52	33'RT	1	1	21
W38	52+10.74	33'RT	1	1	21
W39	52+62.79	32'LT	1	1	45
W40	52+95.08	32'RT	1	1	20
W41	53+92.17	32'RT	1	1	20
W42	54+02.14	34'LT	1	1	47
ITEM TOTALS			43	42	1,428

3

**REMOVING HYDRANTS**

STATION	OFFSET	SPV.0060.08 REMOVING HYDRANTS EA
31+42.00	34.6'LT	1
50+21.70	23.7'LT	1
ITEM TOTALS		2

**REMOVING WATERMAIN**

BEGIN STATION	END STATION	SPV.0090.08 REMOVING EXISTING WATERMAIN LF	REMARKS
21+00.00	21+75.00	155	10-INCH
31+20.00	31+50.00	35	6-INCH
36+65.00	-	30	6-INCH
50+22.00	-	5	6-INCH
ITEM TOTALS		225	

NOTE: ALL ITEMS ON THIS PAGE ARE CATEGORY 0020 UNLESS OTHERWISE NOTED.

PROJECT NO: 7995-02-63

HWY: CTH F

COUNTY: EAU CLAIRE

MISCELLANEOUS QUANTITIES

SHEET

E



**SANITARY SERVICE ITEMS**

SPV.0090.07  
SANITARY  
SERVICE PVC  
4-INCH

NAME	STATION	OFFSET	LF
S1	21+69.01	32' RT	27
S2	22+76.53	32' LT	37
S3	23+29.92	32' RT	27
S4	23+70.00	32' LT	37
S5	24+73.24	32' LT	37
S6	24+96.78	32' RT	27
S7	25+54.94	32' LT	37
S8	26+33.97	32' LT	37
S9	27+11.24	32' RT	27
S10	28+10.78	32' RT	27
S11	28+12.10	32' LT	37
S12	28+68.52	32' RT	27
S13	29+38.57	32' RT	27
S14	29+99.81	32' LT	37
S15	30+35.21	32' RT	27
S16	32+29.61	32' LT	37
S17	34+24.84	32' LT	37
S18	35+66.61	32' LT	37
S19	36+68.07	32' LT	37
S20	37+65.97	32' LT	37
S21	38+92.27	32' LT	37
S22	40+15.57	32' LT	37
S23	41+40.21	32' LT	37
S24	41+91.38	32' RT	27
S25	42+40.09	32' LT	37
S26	42+67.14	32' RT	27
S27	43+21.98	32' RT	27
S28	43+25.12	32' LT	37
S29	43+84.42	32' LT	37
S30	44+00.18	32' RT	27
S31	44+85.90	32' LT	37
S32	44+98.45	34' RT	29
S33	45+79.83	32' LT	37
S34	43+08.87	34' RT	29
S35	46+92.26	34' RT	29
S36	47+65.28	34' RT	29
S37	48+45.92	32' LT	37
S38	49+17.22	34' RT	29
S39	51+07.66	34' RT	33
S40	52+17.09	34' RT	32
S41	52+68.53	32' LT	35
S42	52+88.51	32' RT	29
S43	53+34.08	32' RT	28
S44	53+86.51	32' RT	28
S45	53+92.20	32' LT	37
ITEM TOTALS			1,468

**SANITARY SEWER PIPE ITEMS**

SPV.0090.05 SPV.0090.06  
SANITARY SANITARY  
SEWER PVC SEWER PVC

BEGIN STRUCTURE	END STRUCTURE	8-INCH LF	10-INCH LF
SAN MH 19	SAN MH 17	-	40
SAN MH 17	SAN MH 18	-	40
SAN MH 18	STUB (W)	-	10
SAN MH 17	SAN MH 16	-	251
SAN MH 16	SAN MH 14	-	246
SAN MH 14	SAN MH 15	-	40
SAN MH 15	STUB (W)	-	10
SAN MH 14	SAN MH 13	-	225
SAN MH 13	SAN MH 2	190	241
SAN MH 2	EX SAN MH 1	65	-
SAN MH 2	SAN MH 3	206	-
SAN MH 3	SAN MH 4	46	-
SAN MH 4	STUB (W)	10	-
SAN MH 3	SAN MH 5	300	-
SAN MH 5	SAN MH 6	326	-
SAN MH 6	SAN MH 7	45	-
SAN MH 7	STUB (W)	10	-
SAN MH 6	SAN MH 8	401	-
SAN MH 8	SAN MH 9	350	-
SAN MH 9	SAN MH 10	200	-
EX SAN MH 11	SAN MH 12	381	-
ITEM TOTALS		2530	1103

**SANITARY SEWER STRUCTURE ITEMS**

SPV.0200.01  
SANITARY  
MANHOLES  
& CASTINGS  
4-FT DIAMETER

SPV.0060.09  
SANITARY MANHOLE  
OUTSIDE DROP  
SECTION

SPV.0060.10  
ADJUSTING  
MANHOLE  
COVERS

STRUCTURE	STATION	OFFSET	VF	SECTION EACH	COVERS EACH
19	21+40.00	5.0' RT	17.4	-	-
17	21+80.00	5.0' RT	17.8	-	-
18	21+75.40	33.0' LT	17.1	-	-
16	24+30.00	5.0' RT	20.1	-	-
14	26+75.00	5.0' RT	24.2	-	-
15	26+74.30	35.0' LT	24.4	-	-
13	29+00.00	5.0' RT	29.2	-	-
2	31+40.00	5.0' RT	29.8	1	-
EX 1	31+21.80	33.50' LT	-	-	1
3	33+45.00	5.0' RT	21.6	1	-
4	33+47.50	40.0' LT	17.4	-	-
5	36+45.00	5.0' RT	21.7	-	-
6	39+70.00	5.0' RT	21.1	-	-
7	39+70.00	40.0' LT	19.3	-	-
8	43+70.00	5.0' RT	18.2	-	-
9	47+20.00	5.0' RT	16.3	-	-
10	49+20.00	5.0' RT	16.6	-	-
EX 11	50+14.80	0.3' LT	-	-	1
12	53+95.00	5.0' RT	10.6	-	-
ITEM TOTALS			342.8	2	2

NOTE: ALL ITEMS ON THIS PAGE ARE  
CATEGORY 0020 UNLESS OTHERWISE NOTED.



# EAU CLAIRE COUNTY EAU CLAIRE COUNTY HIGHWAY DEPT TRANSPORTATION PROJECT PLAT TITLE SHEET 7995-02-62 C EAU CLAIRE, STATE ST / CTH F HEATHER ROAD TO HAMILTON AVE CTH F EAU CLAIRE COUNTY

4

4

### CONVENTIONAL SYMBOLS

SECTION LINE	---	SECTION CORNER SYMBOL		R/W MONUMENT (TO BE SET)	●
QUARTER LINE	---	SECTION CORNER MONUMENT	□	NON-MONUMENTED R/W POINT	○
SIXTEENTH LINE	---	GEODETIC SURVEY MONUMENT		FOUND IRON PIN (1-INCH UNLESS NOTED)	IP ○ xxxxx
NEW REFERENCE LINE	---	SIXTEENTH CORNER MONUMENT		SIGN	
NEW R/W LINE	---	OFF-PREMISE SIGN		COMPENSABLE	
EXISTING R/W OR HE LINE	---	NON-COMPENSABLE		NON-COMPENSABLE	
PROPERTY LINE	---	ELECTRIC POLE		TELEPHONE POLE	
LOT, TIE & OTHER MINOR LINES	---	PEDESTAL (LABEL TYPE) (TV, TEL, ELEC, ETC.)		ACCESS RESTRICTED BY ACQUISITION	
SLOPE INTERCEPT	---	NO ACCESS (BY STATUTORY AUTHORITY)		ACCESS RESTRICTED (BY PREVIOUS PROJECT OR CONTROL)	
CORPORATE LIMITS	---	NO ACCESS (NEW HIGHWAY)		PARCEL NUMBER 25	UTILITY NUMBER 40
UNDERGROUND FACILITY (COMMUNICATIONS, ELECTRIC, ETC.)	---	PARALLEL OFFSETS			
NEW R/W (FEE OR HE) (HATCHING VARIES BY OWNER)	---				
TEMPORARY LIMITED EASEMENT AREA	---				
EASEMENT AREA (PERMANENT LIMITED OR RESTRICTED DEVELOPMENT)	---				
TRANSMISSION STRUCTURES	---				
BUILDING TO BE REMOVED	---				
BRIDGE	---				
CULVERT	---				

### CONVENTIONAL ABBREVIATIONS

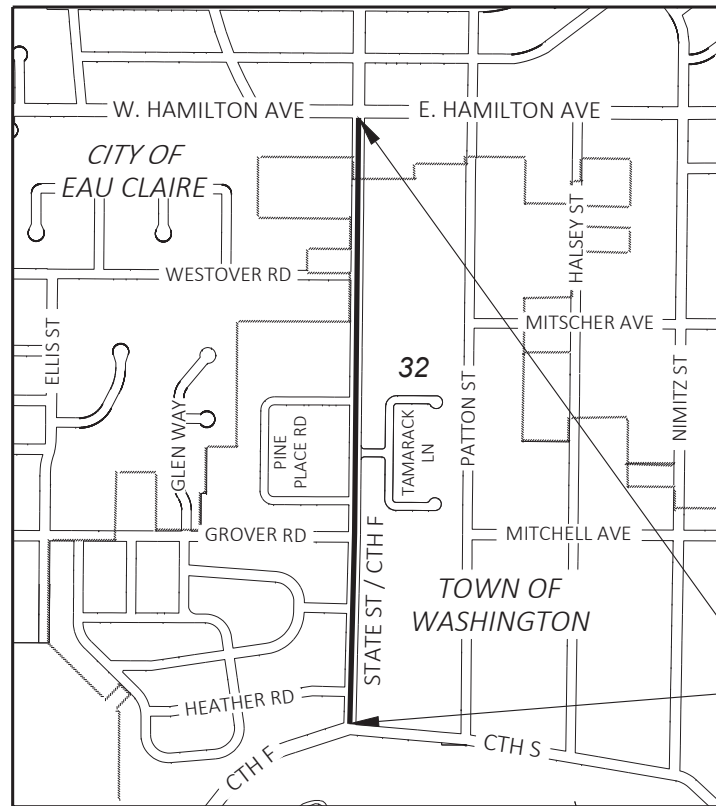
ACCESS RIGHTS	AR	POINT OF COMPOUND CURVE	PCC
ACRES	AC	POINT OF INTERSECTION	PI
AHEAD	AH	PROPERTY LINE	PL
ALUMINUM	ALUM	RECORDED AS (100')	(100')
AND OTHERS	ET AL	REEL / IMAGE	R/I
BACK	BK	REFERENCE LINE	R/L
BLOCK	BLK	REMAINING	REM
CENTERLINE	C/L	RESTRICTIVE DEVELOPMENT	RDE
CERTIFIED SURVEY MAP	CSM	EASEMENT	
CONCRETE	CONC	RIGHT	RT
COUNTY	CO	RIGHT OF WAY	R/W
COUNTY TRUNK HIGHWAY	CTH	SECTION	SEC
DISTANCE	DIST	SEPTIC VENT	SEPV
CORNER	COR	SQUARE FEET	SF
DOCUMENT NUMBER	DOC	STATE TRUNK HIGHWAY	STH
EASEMENT	EASE	STATION	STA
EXISTING	EX	TELEPHONE PEDESTAL	TP
GAS VALVE	GV	TEMPORARY LIMITED	TLE
GRID NORTH	GN	EASEMENT	
HIGHWAY EASEMENT	HE	TRANSPORTATION PROJECT PLAT	TPP
IDENTIFICATION	ID	UNITED STATES HIGHWAY	USH
LAND CONTRACT	LC	VOLUME	V
LEFT	LT		
MONUMENT	MON		
NATIONAL GEODETIC SURVEY	NGS		
NUMBER	NO		
OUTLOT	OL		
PAGE	P		
POINT OF TANGENCY	PT		
PERMANENT LIMITED EASEMENT	PLE		
POINT OF BEGINNING	POB		
POINT OF CURVATURE	PC		

### CURVE DATA ABBREVIATIONS

LONG CHORD	LCH
LONG CHORD BEARING	LCB
RADIUS	R
DEGREE OF CURVE	D
CENTRAL ANGLE	Δ/DELTA
LENGTH OF CURVE	L
TANGENT	T
DIRECTION AHEAD	DA
DIRECTION BACK	DB

### CONVENTIONAL UTILITY SYMBOLS

WATER	---
GAS	---
TELEPHONE	---
OVERHEAD TRANSMISSION LINES	---
ELECTRIC	---
CABLE TELEVISION	---
FIBER OPTIC	---
SANITARY SEWER	---
STORM SEWER	---
ELECTRIC TOWER	⊠



T 27 N

PROJECT LOCATION

R 9 W

LAYOUT  
SCALE 0 600 FEET

THE NOTES, CONVENTIONAL SIGNS, AND ABBREVIATIONS ARE ASSOCIATED WITH EACH TRANSPORTATION PROJECT PLAT FOR PROJECT 7995-02-62.

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), EAU CLAIRE COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 3/4" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

ALL RIGHT-OF-WAY LINES DEPICTED IN THE NON-ACQUISITION AREAS ARE INTENDED TO RE-ESTABLISH EXISTING RIGHT-OF-WAY LINES AS DETERMINED FROM PREVIOUS PROJECTS, OTHER RECORDED DOCUMENTS, CENTERLINE OF EXISTING PAVEMENTS AND/OR EXISTING OCCUPATIONAL LINES.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS" OF PUBLIC RECORD.

DIMENSIONING FOR THE NEW RIGHT-OF-WAY IS MEASURED ALONG AND PERPENDICULAR TO THE NEW REFERENCE LINES.

A TEMPORARY LIMITED EASEMENT (TLE) IS A RIGHT FOR CONSTRUCTION PURPOSES, AS DEFINED HEREIN, INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON, THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM DESIRABLE. ALL (TLEs) ON THIS PLAT EXPIRE AT THE COMPLETION OF THE CONSTRUCTION PROJECT FOR WHICH THIS INSTRUMENT IS GIVEN.

PROPERTY LINES SHOWN ON THIS PLAT FOR PROPERTIES BEING IMPACTED ARE DRAWN FROM DATA DERIVED FROM FILED/RECORDED MAPS AND DOCUMENTS OF PUBLIC RECORD. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.

FOR THE CURRENT ACCESS/DRIVEWAY INFORMATION, CONTACT THE EAU CLAIRE COUNTY HIGHWAY DEPARTMENT OFFICE IN ALTOONA.

PARCEL AND UTILITY IDENTIFICATION NUMBERS MAY NOT POINT TO ALL AREAS OF ACQUISITION, AS NOTED ON THE TPP DETAIL PAGES.

INFORMATION FOR THE BASIS OF EXISTING HIGHWAY RIGHT-OF-WAY POINTS OF REFERENCE AND ACCESS CONTROL ARE LISTED ON THE TPP DETAIL PAGES.

PROJECT NUMBER 7995-02-62-4.01  
SHEET 2 OF 2

**TRANSPORTATION PROJECT PLAT NO: 7995-02-62-4.01 AMENDMENT NO: 2**  
**REMOVES PARCEL 9 AND ADDS UTILITY NO: 82 AND AMENDS PARCEL NO: 1, 2 AND 3 TO**  
**TRANSPORTATION PROJECT PLAT 7995-02-62-4.01 AMENDMENT NO:1 RECORDED AS**  
**DOCUMENT NO: 1245739**

THAT PART OF LOT 5, BLOCK 10, PART OF LOT 10, BLOCK 8, AND PART OF LOT 7, BLOCK 4, ALL IN CORYDON PARK ADDITION, AND PART OF LOTS 54, 55 AND 56, ALL IN FIRST ASSESSOR'S PLAT TOWN OF WASHINGTON, PART OF LOT 1, CERTIFIED SURVEY MAP 2985, V. 16, PG. 404, DOC. 1101372, ALL LOCATED IN PART OF THE NW 1/4 OF THE SE 1/4, SECTION 32, TOWNSHIP 27 NORTH, RANGE 9 WEST, TOWN OF WASHINGTON, EAU CLAIRE COUNTY, WISCONSIN.

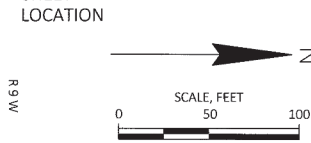
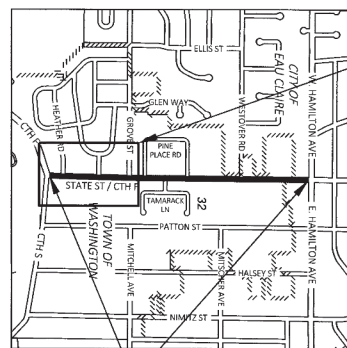
RELOCATION ORDER CTH F C EAU CLAIRE, STATE ST / CTH F, HEATHER ROAD TO HAMILTON AVE, EAU CLAIRE COUNTY

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE NAMED PROJECT.

TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SECTION 83.07 AND 83.08, WISCONSIN STATUTES, EAU CLAIRE COUNTY HEREBY ORDERS THAT:

- THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS LAID OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE PROJECT.
- THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE COUNTY FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE COUNTY OF EAU CLAIRE, PURSUANT TO THE PROVISIONS OF SECTION 83.07 AND 83.08, WISCONSIN STATUTES.

**LOCATION SKETCH**  
NOT TO SCALE



PROJECT LOCATION

FOR ADDITIONAL INFORMATION REFER TO THE TITLE SHEET, RECORDED IN THE OFFICE OF THE REGISTER OF DEEDS IN EAU CLAIRE COUNTY AS SHEET 2 OF 2 DOCUMENT NUMBER 1243895, VOL. 2 TPP, PAGE 1.

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), EAU CLAIRE COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 3/4" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

FOR THE CURRENT ACCESS/DRIVEWAY INFORMATION, CONTACT THE EAU CLAIRE COUNTY HIGHWAY DEPARTMENT OFFICE IN ALTOONA.

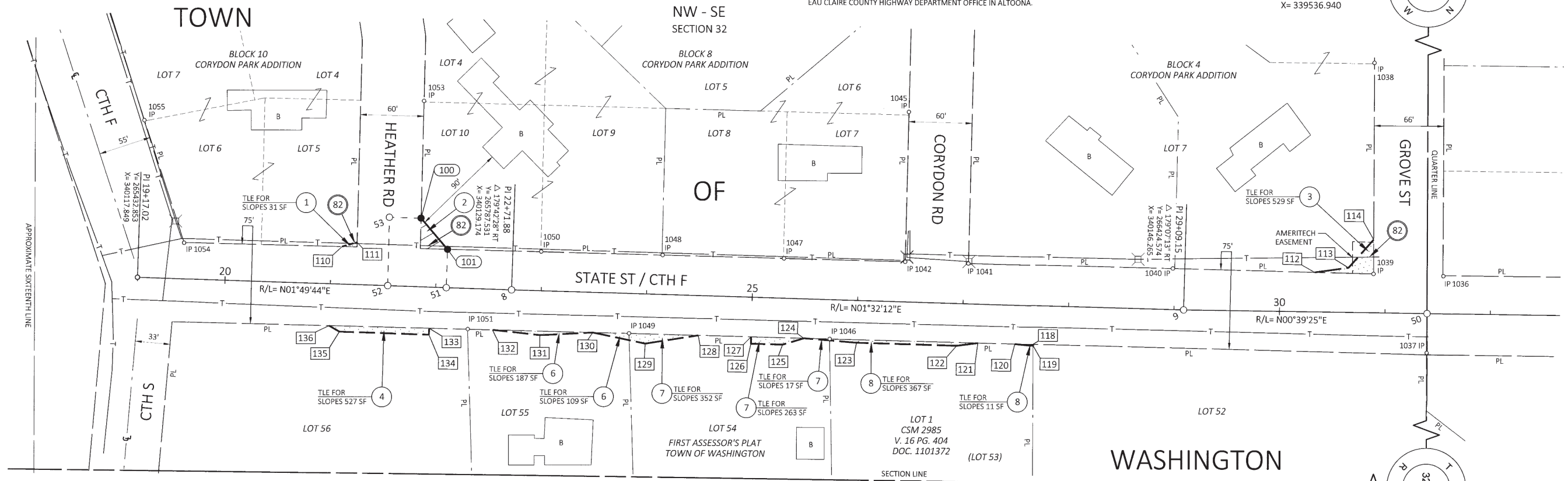
**SCHEDULE OF LANDS & INTERESTS REQUIRED**

PARCEL NUMBER	OWNER(S)	INTERESTS REQUIRED	R/W SQUARE FEET REQUIRED			TLE SQUARE FEET
			NEW	EXISTING	TOTAL	
1	MICHAEL A. & CHERYL A. CORNEILLER REV LIV TRUST	TLE	-	-	-	31
2	SHUVRA & KRISTI L. KNOCH GUPTA	FEE	373	-	373	-
3	MICHAEL KERN & SARAH KERVIN	TLE	-	-	-	529
4	JASON R. & MARIE L. ALBRECHT	TLE	-	-	-	527
6	JASON M. & GAYLE L. CZUBAKOWSKI	TLE	-	-	-	296
7	KENNETH G. FULGIONE	TLE	-	-	-	632
8	TIMOTHY J. & CARLEE L. O'CONNOR	TLE	-	-	-	378

ROAD NAME	BASIS OF EXISTING R/W	YEAR
STATE ST / CTH F,	CSM 2985, V. 16, PG. 404, DOC. 1101372	2012
STATE ST / CTH F, HEATHER RD, CORYDON RD, GROVE ST	CORYDON PARK ADDITION	1959
STATE ST / CTH F, CTH S, GROVE ST	FIRST ASSESSOR'S PLAT TOWN OF WASHINGTON	1955

2 FOUND ALUMINUM CAP  
Y= 266657.740  
X= 339536.940

3 FOUND ALUMINUM CAP  
Y= 266647.370  
X= 342176.040



POINT	Y	X	DESCRIPTION	STATION	OFFSET
1036	266671.002	340113.635	2" IRON PIPE HIT	31+55.19	35.45' LT
1037	266655.111	340186.216	1" REBAR	31+40.13	37.31' RT
1038	266605.401	339911.871	REBAR	30+87.28	236.45' LT
1039	266604.584	340111.785	REBAR	30+88.76	36.54' LT
1040	266414.498	340107.251	REBAR	28+98.03	38.73' LT
1041	266220.476	340102.635	2" IRON PIPE	27+03.96	38.14' LT
1042	266160.548	340101.242	2" IRON PIPE	26+44.01	37.93' LT
1045	266163.820	339960.949	ELEC BOX	26+43.52	178.25' LT
1046	266089.063	340174.482	1" IRON PIPE	25+74.52	37.21' RT
1047	266045.753	340098.485	1" IRON PIPE	25+29.19	37.60' LT
1048	265930.839	340095.740	1" IRON PIPE	24+14.24	37.26' LT
1049	265898.996	340169.856	1.5" IRON PIPE	23+84.40	37.68' RT
1050	265815.651	340092.835	1" IRON PIPE	22+99.02	37.08' LT
1051	265746.063	340166.246	1" IRON PIPE	22+31.62	38.38' RT
1053	265704.406	339950.266	1" IRON PIPE	21+83.09	176.16' LT
1054	265477.186	340085.157	1/2" REBAR	19+60.29	34.09' LT
1055	265439.345	339968.909	1" IRON PIPE	19+18.76	149.07' LT

COURSE	BEARING	DISTANCE
3 - 50	N 89° 46' 30" W	2027.14'
50 - 9	S 00° 39' 25" W	230.78'
9 - 8	S 01° 32' 12" W	637.27'
8 - 51	S 01° 49' 44" W	61.88'
51 - 52	S 01° 49' 44" W	55.60'
52 - 53	N 88° 35' 50" W	64.78'
53 - 100	N 01° 24' 10" E	30.00'
100 - 101	N 49° 31' 19" E	38.75'
101 - 51	S 88° 10' 16" E	36.34'

COURSE	BEARING	DISTANCE
50 - 2	N 89° 46' 30" W	611.98'
3 - 2	N 89° 46' 30" W	2639.12'

POINT	STATION	OFFSET	POINT	STATION	OFFSET
110	21+10.00	35.53' LT	125	25+30.00	44.00' RT
111	21+24.10	40.00' LT	126	25+00.00	44.00' RT
112	30+33.00	37.23' LT	127	25+00.00	37.47' RT
8 - 51	30+88.52	69.00' LT	128	24+50.00	37.62' RT
114	30+88.52	69.00' LT	129	24+00.00	47.00' RT
118	27+67.40	36.67' RT	130	23+50.00	37.93' RT
119	27+67.43	38.00' RT	131	23+00.00	42.00' RT
120	27+50.00	36.72' RT	132	22+55.00	38.30' RT
121	27+15.00	36.83' RT	133	21+95.00	38.78' RT
122	26+95.00	40.00' RT	134	21+95.00	45.00' RT
123	26+00.00	40.00' RT	135	21+10.00	45.00' RT
124	25+50.00	37.32' RT	136	21+00.00	39.55' RT

POINT	STATION	OFFSET
50	31+39.93	0.00'
51	22+10.00	0.00'
52	21+54.40	0.00'
53	21+53.92	64.78' LT
100	21+83.91	65.00' LT
101	22+10.00	36.34' LT

UTILITY NUMBER	OWNER (S)	INTEREST REQUIRED
80	NORTHERN STATES POWER d/b/a XCEL ENERGY	RELEASE OF RIGHTS
81	AMERITECH d/b/a/ AT&T	RELEASE OF RIGHTS
82	CHARTER COMMUNICATIONS	RELEASE OF RIGHTS

UTILITY NUMBER	PARCEL	RECORDING INFORMATION	NAME
80	3	DOC. 319773	d/b/a XCEL ENERGY
81	3	DOC. 781872	d/b/a AT&T
82	1, 2, 3	NO RECORD OF EASEMENT	CHARTER COMMUNICATIONS

I, JASON L. CANCE PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF EAU CLAIRE COUNTY, I HAVE SURVEYED AND MAPPED THIS TRANSPORTATION PROJECT PLAT AND SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.

SIGNATURE: [Signature] DATE: 1-16-24  
 PRINT NAME: JASON L. CANCE  
 REGISTRATION NUMBER: S-2688

THIS PLAT AND RELOCATION ORDER ARE APPROVED FOR EAU CLAIRE COUNTY

SIGNATURE: [Signature] DATE: 1-17-24  
 PRINT NAME: JON JOHNSON



**TRANSPORTATION PROJECT PLAT NO: 7995-02-62-4.02 AMENDMENT NO: 2**  
**ADDS UTILITY NO: 82 AND 83 AND AMENDS PARCEL NO: 18, 23 AND 26 TO TRANSPORTATION**  
**PROJECT PLAT 7995-02-62-4.02 AMENDMENT NO: 1 RECORDED AS DOCUMENT NO: 1245740**

THAT PART OF LOTS 1 AND 20, BLOCK 1, AND PART OF LOTS 1 AND 11, BLOCK 2, PINE PLACE ADDITION, AND PART OF LOTS 47, 48, 50 AND 51, FIRST ASSESSOR'S PLAT TOWN OF WASHINGTON, AND PART OF LOTS 1, 3, 4, 26, 27, 28, AND 29, TAMARAK SUBDIVISION PLAT, ALL LOCATED IN PART OF THE SW 1/4 OF THE NE 1/4, SECTION 32, TOWNSHIP 27 NORTH, RANGE 9 WEST, TOWN OF WASHINGTON, EAU CLAIRE COUNTY, WISCONSIN.

RELOCATION ORDER CTH F C EAU CLAIRE, STATE ST / CTH F, HEATHER ROAD TO HAMILTON AVE, EAU CLAIRE COUNTY

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE NAMED PROJECT.

TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SECTION 83.07 AND 83.08, WISCONSIN STATUTES, EAU CLAIRE COUNTY HEREBY ORDERS THAT:  
 1. THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS LAID OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE PROJECT.  
 2. THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE COUNTY FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE COUNTY OF EAU CLAIRE, PURSUANT TO THE PROVISIONS OF SECTION 83.07 AND 83.08, WISCONSIN STATUTES.

**LOCATION SKETCH**  
NOT TO SCALE



SHEET LOCATION

PROJECT LOCATION

FOR ADDITIONAL INFORMATION REFER TO THE TITLE SHEET, RECORDED IN THE OFFICE OF THE REGISTER OF DEEDS IN EAU CLAIRE COUNTY AS SHEET 2 OF 2 DOCUMENT NUMBER 1243895, VOL. 2 TPP, PAGE 1.

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCONS), EAU CLAIRE COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 3/4" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

FOR THE CURRENT ACCESS/DRIVEWAY INFORMATION, CONTACT THE EAU CLAIRE COUNTY HIGHWAY DEPARTMENT OFFICE IN ALTOONA.

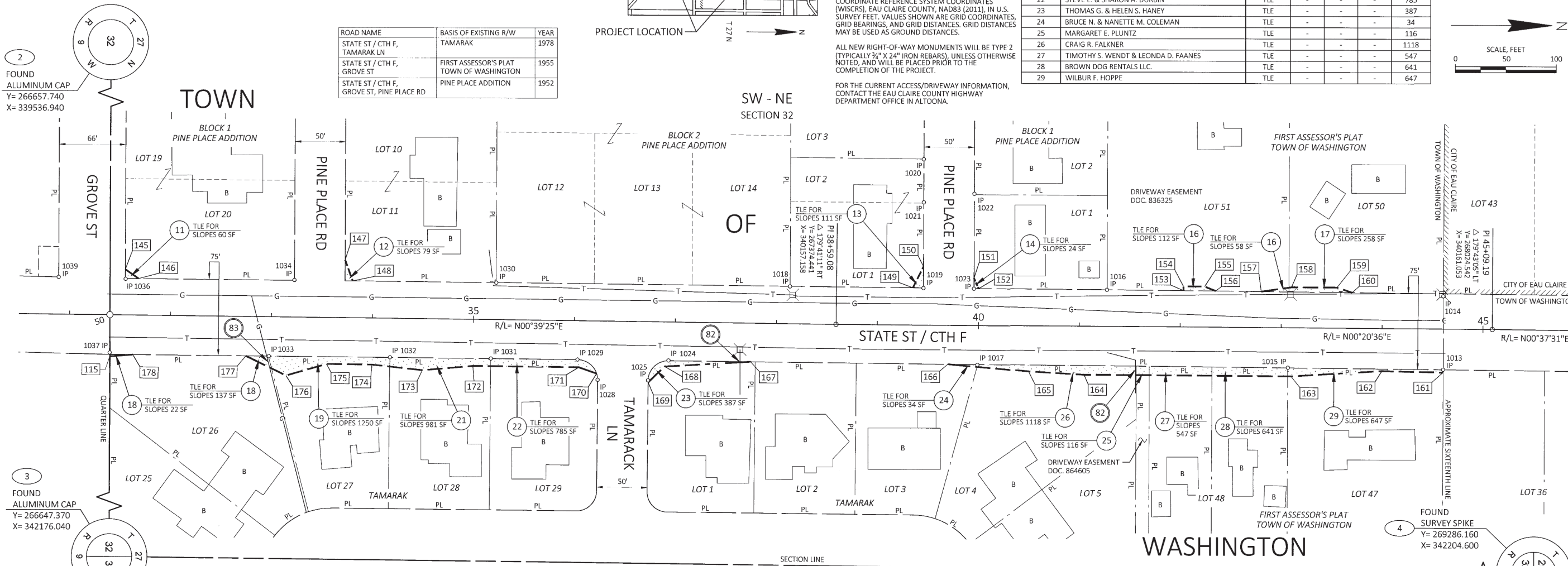
**SCHEDULE OF LANDS & INTERESTS REQUIRED**

PARCEL NUMBER	OWNER(S)	INTERESTS REQUIRED	R/W SQUARE FEET REQUIRED			TLE SQUARE FEET
			NEW	EXISTING	TOTAL	
11	DONALD BRUNING	TLE	-	-	-	60
12	SONNENBERG REVOCABLE TRUST	TLE	-	-	-	79
13	RANDALL A. YEAST II & MANDI M. YEAST	TLE	-	-	-	111
14	NATHAN K. ADAMS & STEPHANIE A. WILCZYNSKI	TLE	-	-	-	24
16	TINA KAYE SCHROEDER	TLE	-	-	-	170
17	MICHAEL D. & LESLIE R. STEINKE	TLE	-	-	-	258
18	DANIEL F. JOHNSON	TLE	-	-	-	159
19	BRIAN R. & SHARLENE SCHILLING	TLE	-	-	-	1250
21	CEARA MARY O'CARROLL & RODRIGO DIAZ-GARCIA	TLE	-	-	-	981
22	STEVE E. & SHARON A. DURBIN	TLE	-	-	-	785
23	THOMAS G. & HELEN S. HANEY	TLE	-	-	-	387
24	BRUCE N. & NANETTE M. COLEMAN	TLE	-	-	-	34
25	MARGARET E. PLUNTZ	TLE	-	-	-	116
26	CRAIG R. FALKNER	TLE	-	-	-	1118
27	TIMOTHY S. WENDT & LEONDA D. FAANES	TLE	-	-	-	547
28	BROWN DOG RENTALS LLC.	TLE	-	-	-	641
29	WILBUR F. HOPPE	TLE	-	-	-	647

VOL 2 TPP PAGE 58

1251513  
 TINA K. POMMER  
 EAU CLAIRE COUNTY WI  
 REGISTER OF DEEDS  
 RECORDED ON  
 01/18/2024 08:00 AM  
 REC FEE: \$25.00  
 TRANS FEE:  
 EXEMPT #:  
 PAGES: 1

RESERVED FOR REGISTER OF DEEDS  
 PROJECT NUMBER 7995-02-62-4.02  
 AMENDMENT NO: 2



ROAD NAME	BASIS OF EXISTING R/W	YEAR
STATE ST / CTH F, TAMARAK LN	TAMARAK	1978
STATE ST / CTH F, GROVE ST	FIRST ASSESSOR'S PLAT TOWN OF WASHINGTON	1955
STATE ST / CTH F, GROVE ST, PINE PLACE RD	PINE PLACE ADDITION	1952

UTILITY EASEMENT TABLE			
UTILITY NUMBER	PARCEL	RECORDING INFORMATION	NAME
82	23, 26	NO RECORD OF EASEMENT	CHARTER COMMUNICATIONS
83	18	NO RECORD OF EASEMENT	XCEL ENERGY

UTILITY INTERESTS REQUIRED		
UTILITY NUMBER	OWNER(S)	INTEREST REQUIRED
82	CHARTER COMMUNICATIONS	RELEASE OF RIGHTS
83	XCEL ENERGY - GAS	RELEASE OF RIGHTS

FEE STATION & OFFSET TABLE		
POINT	STATION	OFFSET
50	31+39.93	0.00'

RECOVERED MONUMENTS											
POINT	Y	X	DESCRIPTION	STATION	OFFSET	POINT	Y	X	DESCRIPTION	STATION	OFFSET
1013	267975.670	340200.860	1" IRON PIPE	44+60.56	40.10' RT	1025	267188.378	340213.033	REBAR	36+73.67	58.00' RT
1014	267975.958	340128.493	1" IRON PIPE	44+60.42	32.27' LT	1028	267138.467	340212.630	1" IRON PIPE	36+23.76	58.17' RT
1015	267821.980	340199.303	1" IRON PIPE	43+06.87	39.46' RT	1029	267118.568	340192.510	REBAR	36+03.63	38.28' RT
1016	267642.819	340122.750	2" IRON PIPE	41+27.25	36.02' LT	1030	267038.021	340116.888	1" IRON PIPE	35+22.22	36.41' LT
1017	267514.443	340196.688	1" IRON PIPE W/CAP	39+99.32	38.69' RT	1031	267032.582	340191.699	1" IRON PIPE	35+17.64	38.46' RT
1018	267328.942	340119.775	1" IRON PIPE	38+13.16	36.86' LT	1032	266932.629	340190.740	1" IRON PIPE	34+17.68	38.65' RT
1019	267461.004	340121.097	2" IRON PIPE	39+45.43	36.58' LT	1033	266812.758	340189.601	1" IRON PIPE	32+97.81	38.88' RT
1020	267461.477	339993.536	1" IRON PIPE	39+45.14	164.14' LT	1034	266838.210	340114.551	2" IRON PIPE	33+22.40	36.45' LT
1021	267461.412	340035.988	1" IRON PIPE	39+45.33	121.69' LT	1036	266671.002	340113.635	2" IRON PIPE HIT	31+55.19	35.45' LT
1022	267511.508	340027.480	1" IRON PIPE	39+95.37	130.50' LT	1037	266655.111	340186.216	1" REBAR	31+40.13	37.30' RT
1023	267511.060	340121.531	2" IRON PIPE	39+95.49	36.44' LT	1039	266604.584	340111.785	REBAR	30+88.76	36.54' LT
1024	267208.634	340193.231	1" IRON PIPE	36+93.70	37.97' RT						

TLE STATION & OFFSET TABLE								
POINT	STATION	OFFSET	POINT	STATION	OFFSET	POINT	STATION	OFFSET
115	31+40.24	41.00' RT	155	42+24.00	40.00' LT	167	37+75.00	38.03' RT
145	31+54.69	45.00' LT	156	42+35.00	35.68' LT	168	36+90.00	44.00' RT
146	31+68.00	35.95' LT	157	42+80.00	35.51' LT	169	36+73.74	60.00' RT
147	33+72.25	57.00' LT	158	43+10.00	40.00' LT	170	36+22.71	52.00' RT
148	33+80.00	36.30' LT	159	43+55.00	40.00' LT	171	36+05.00	46.00' RT
149	39+35.00	36.82' LT	160	43+70.00	35.17' LT	172	35+00.00	46.00' RT
150	39+45.42	58.00' LT	161	44+60.43	43.00' RT	173	34+50.00	51.00' RT
151	39+95.44	47.00' LT	162	44+00.00	42.00' RT	174	34+00.00	46.00' RT
152	40+00.00	36.58' LT	163	43+10.00	48.00' RT	175	33+55.00	46.00' RT
153	42+03.00	35.81' LT	164	41+05.00	48.00' RT	176	33+15.00	58.00' RT
154	42+03.00	40.00' LT	165	40+50.00	45.00' RT	177	32+75.00	38.87' RT
			166	39+70.00	38.31' RT	178	31+63.00	39.06' RT

QUARTER LINE - COURSE TABLE		
COURSE	BEARING	DISTANCE
3 - 50	N 89° 46' 30" W	2027.14'
50 - 2	N 89° 46' 30" W	611.98'
3 - 2	N 89° 46' 30" W	2639.12'

SECTION LINE - COURSE TABLE		
COURSE	BEARING	DISTANCE
3 - 4	N 00° 37' 12" E	2638.94'

I, JASON L. CANCE PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF EAU CLAIRE COUNTY, I HAVE SURVEYED AND MAPPED THIS TRANSPORTATION PROJECT PLAT AND SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.

SIGNATURE: DATE: 1-16-24  
 PRINT NAME: JASON L. CANCE  
 REGISTRATION NUMBER: S-2688

THIS PLAT AND RELOCATION ORDER ARE APPROVED FOR EAU CLAIRE COUNTY

SIGNATURE: DATE: 1-17-24  
 PRINT NAME: JON JOHNSON



**TRANSPORTATION PROJECT PLAT NO: 7995-02-62-4.03 AMENDMENT NO: 1**  
**AMENDS PARCEL NO: 31 AND 32 TO TRANSPORTATION PROJECT PLAT**  
**7995-02-62-4.03 RECORDED AS DOCUMENT NO: 1244033**

THAT PART OF LOT 2, CERTIFIED SURVEY MAP 1319, V. 7, PG. 61, DOC. NO. 707513, AND PART OF LOT 26, FIRST ASSESSOR'S PLAT TOWN OF WASHINGTON, LOCATED IN PART OF THE NW 1/4 OF THE NE 1/4, SECTION 32, TOWNSHIP 27 NORTH, RANGE 9 WEST, CITY OF EAU CLAIRE, AND PART OF LOTS 27, 28, 29, 33, 34, 35, 36, 38 AND 40, FIRST ASSESSOR'S PLAT TOWN OF WASHINGTON, AND PART OF CERTIFIED SURVEY MAP 2823, V. 16, PG. 46, DOC. 1064189, LOCATED IN PART OF THE NW 1/4 OF THE NE 1/4, SECTION 32, TOWNSHIP 27 NORTH, RANGE 9 WEST, TOWN OF WASHINGTON, EAU CLAIRE COUNTY, WISCONSIN.

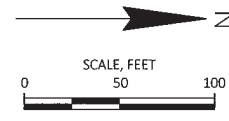
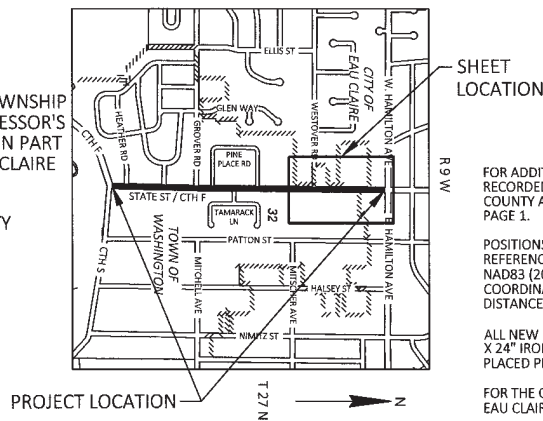
RELOCATION ORDER CTH F C EAU CLAIRE, STATE ST / CTH F, HEATHER ROAD TO HAMILTON AVE, EAU CLAIRE COUNTY

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE NAMED PROJECT.

TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SECTION 83.07 AND 83.08, WISCONSIN STATUTES, EAU CLAIRE COUNTY HEREBY ORDERS THAT:

- THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS LAID OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE PROJECT.
- THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE COUNTY FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE COUNTY OF EAU CLAIRE, PURSUANT TO THE PROVISIONS OF SECTION 83.07 AND 83.08, WISCONSIN STATUTES.

**LOCATION SKETCH**  
NOT TO SCALE



FOR ADDITIONAL INFORMATION REFER TO THE TITLE SHEET, RECORDED IN THE OFFICE OF THE REGISTER OF DEEDS IN EAU CLAIRE COUNTY AS SHEET 2 OF 2 DOCUMENT NUMBER 1243895, VOL. 2 TPP, PAGE 1.

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), EAU CLAIRE COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 3/4" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

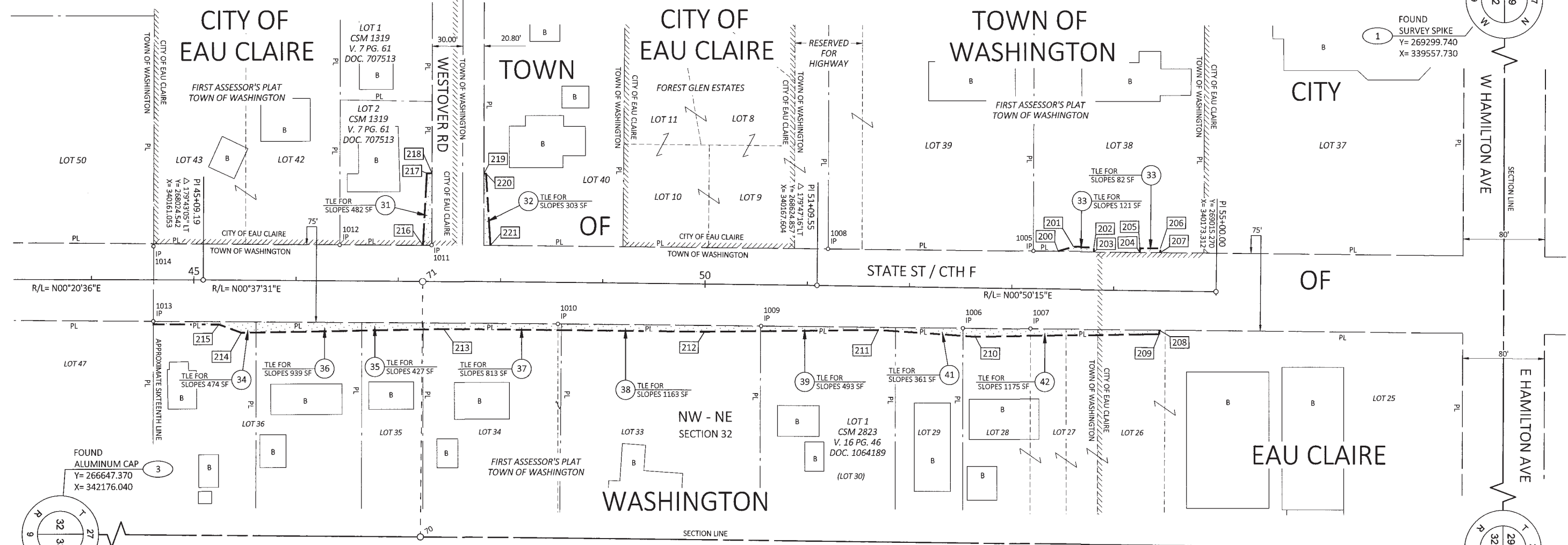
FOR THE CURRENT ACCESS/DRIVEWAY INFORMATION, CONTACT THE EAU CLAIRE COUNTY HIGHWAY DEPARTMENT OFFICE IN ALTOONA.

**SCHEDULE OF LANDS & INTERESTS REQUIRED**

PARCEL NUMBER	OWNER(S)	INTERESTS REQUIRED	R/W SQUARE FEET REQUIRED			TLE SQUARE FEET
			NEW	EXISTING	TOTAL	
31	RON A. KOPP	TLE	-	-	-	482
32	KAJIA A. JOHNSON	TLE	-	-	-	303
33	BENJAMIN T. CHENEY GST EXEMPT TRUST	TLE	-	-	-	203
34	GAYLEN E. & BEVERLY J. JOHNSON	TLE	-	-	-	474
35	MICHAEL D. & BEBEANNA B. BUCK	TLE	-	-	-	427
36	GREG G. LINDNER	TLE	-	-	-	939
37	ANDREW J. & REBECCA L. BUCK	TLE	-	-	-	813
38	ROBERT L. & SUSAN M. SAXE	TLE	-	-	-	1163
39	JOSEPH C. STROMBERG & COURTNY C. POST	TLE	-	-	-	493
41	PETER H. & CAROL A. ANDERSON	TLE	-	-	-	361
42	ST. MARK EVANGELICAL LUTHERAN CHURCH	TLE	-	-	-	1175

1245741  
 TINA K. POMNIER  
 EAU CLAIRE COUNTY WI  
 REGISTER OF DEEDS  
 RECORDED ON  
 07/18/2023 02:32 PM  
 REC. FEE: \$25.00  
 TRANS. FEE:  
 EXEMPT #1  
 PAGES: 1

RESERVED FOR REGISTER OF DEEDS  
 PROJECT NUMBER 7995-02-62-4.03  
 AMENDMENT NO: 1



COMPUTED POINT STATION & OFFSET TABLE

POINT	STATION	OFFSET
70	47+23.84	2029.75' RT
71	47+23.84	0.00'

RECOVERED MONUMENTS

POINT	Y	X	DESCRIPTION	STATION	OFFSET
1005	268836.702	340134.365	1" IRON PIPE	53+20.88	36.33' LT
1006	268767.500	340208.682	1" IRON PIPE	52+52.77	38.99' RT
1007	268833.526	340209.209	NAIL	53+18.80	38.55' RT
1008	268635.760	340132.377	1" IRON PIPE	51+19.93	35.38' LT
1009	268570.218	340206.795	1" IRON PIPE	50+55.34	39.78' RT
1010	268371.514	340204.813	1" IRON PIPE	48+56.62	39.97' RT
1011	268248.008	340128.327	3/4" REBAR	47+32.29	35.16' LT
1012	268158.168	340127.892	1" IRON PIPE	46+42.45	34.62' LT
1013	267975.670	340200.860	1" IRON PIPE	44+60.56	40.10' RT
1014	267975.958	340128.493	1" IRON PIPE	44+60.42	32.27' LT

TLE STATION & OFFSET TABLE

POINT	STATION	OFFSET
200	53+45.00	36.46' LT
201	53+60.00	41.00' LT
202	53+80.00	41.00' LT
203	53+80.00	36.63' LT
204	54+25.00	36.84' LT
205	54+25.00	41.00' LT
206	54+45.00	41.00' LT
207	54+45.00	36.94' LT
208	54+45.00	38.06' RT
209	54+45.00	42.00' RT
210	52+65.00	47.00' RT

TLE STATION & OFFSET TABLE

POINT	STATION	OFFSET
211	51+70.00	42.00' RT
212	50+00.00	46.00' RT
213	47+45.00	46.00' RT
214	45+47.00	51.00' RT
215	45+25.00	43.00' RT
216	47+23.84	34.88' LT
217	47+27.00	105.00' LT
218	47+32.00	105.00' LT
219	47+82.85	105.00' LT
220	47+85.00	105.00' LT
221	47+89.84	34.95' LT

SECTION LINE - COURSE TABLE

COURSE	BEARING	DISTANCE
3 - 70	N 00° 37' 12" E	1569.74'
70 - 4	N 00° 37' 12" E	1069.20'
3 - 4	N 00° 37' 12" E	2638.94'
70 - 71	N 89° 22' 29" W	2029.75'
4 - 1	N 89° 42' 22" W	2646.90'

ROAD NAME BASIS OF EXISTING R/W YEAR

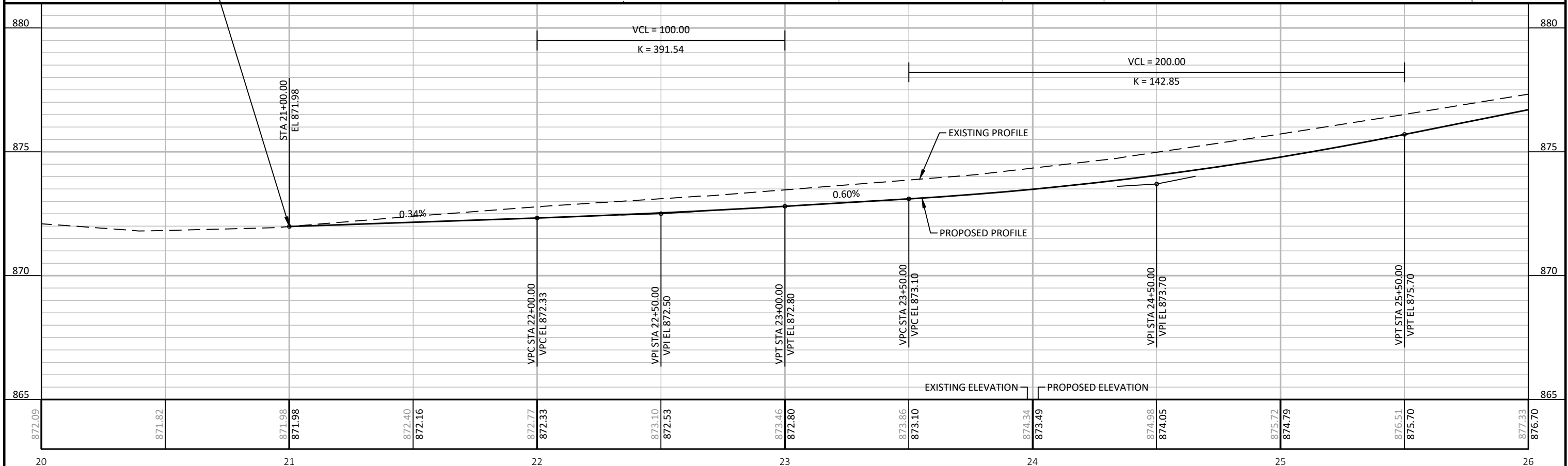
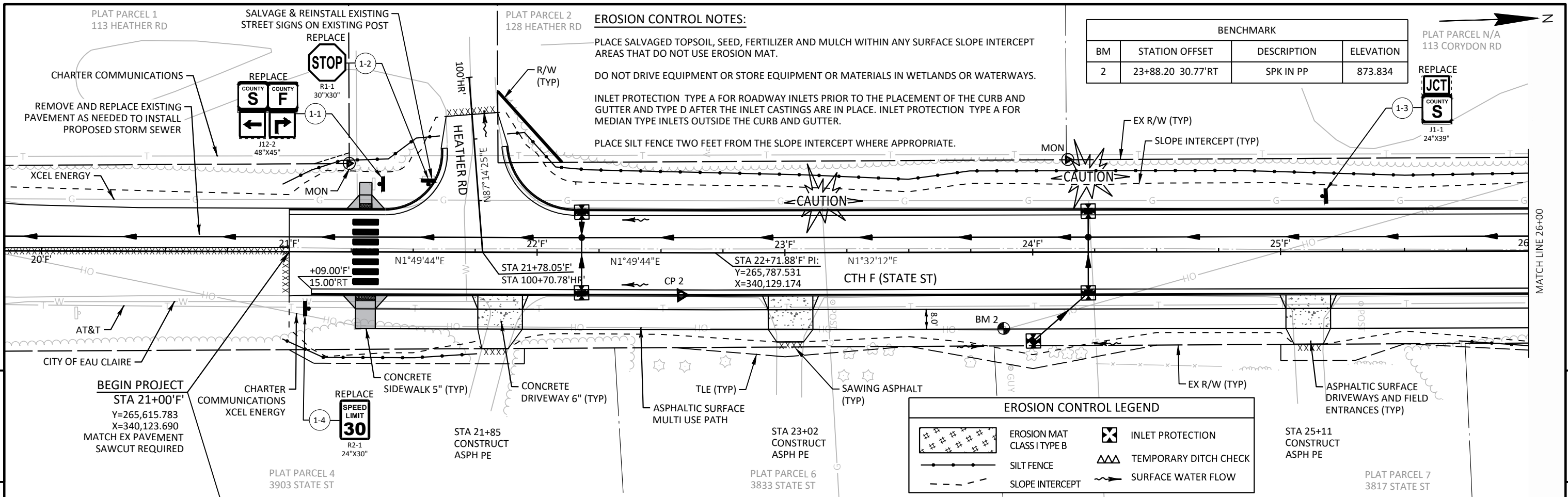
STATE ST / CTH F, WESTOVER RD	FOREST GLEN ESTATES	2018
STATE ST / CTH F	CSM 2823, V. 16, PG. 46, DOC. 1064189	2012
STATE ST / CTH F	CSM 1319, V. 7, PG. 61, DOC. 707513	1996
STATE ST / CTH F, WESTOVER AVE (FKA TIETZ AVE), HAMILTON AVE	FIRST ASSESSOR'S PLAT TOWN OF WASHINGTON	1955

I, JASON L. CANCE PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF EAU CLAIRE COUNTY, I HAVE SURVEYED AND MAPPED THIS TRANSPORTATION PROJECT PLAT AND SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.

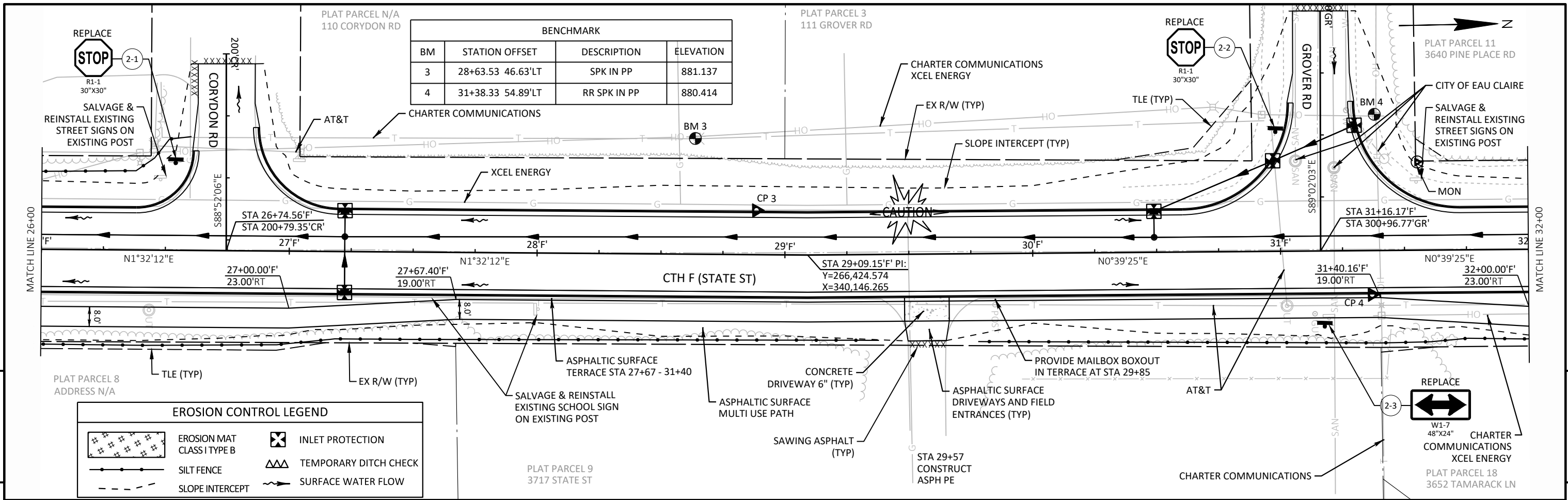


SIGNATURE: *[Signature]* DATE: 7-18-23  
 PRINT NAME: JASON L. CANCE  
 REGISTRATION NUMBER: S-2688  
 THIS PLAT AND RELOCATION ORDER ARE APPROVED FOR EAU CLAIRE COUNTY  
 SIGNATURE: *[Signature]* DATE: 7-18-23  
 PRINT NAME: JON JOHNSON



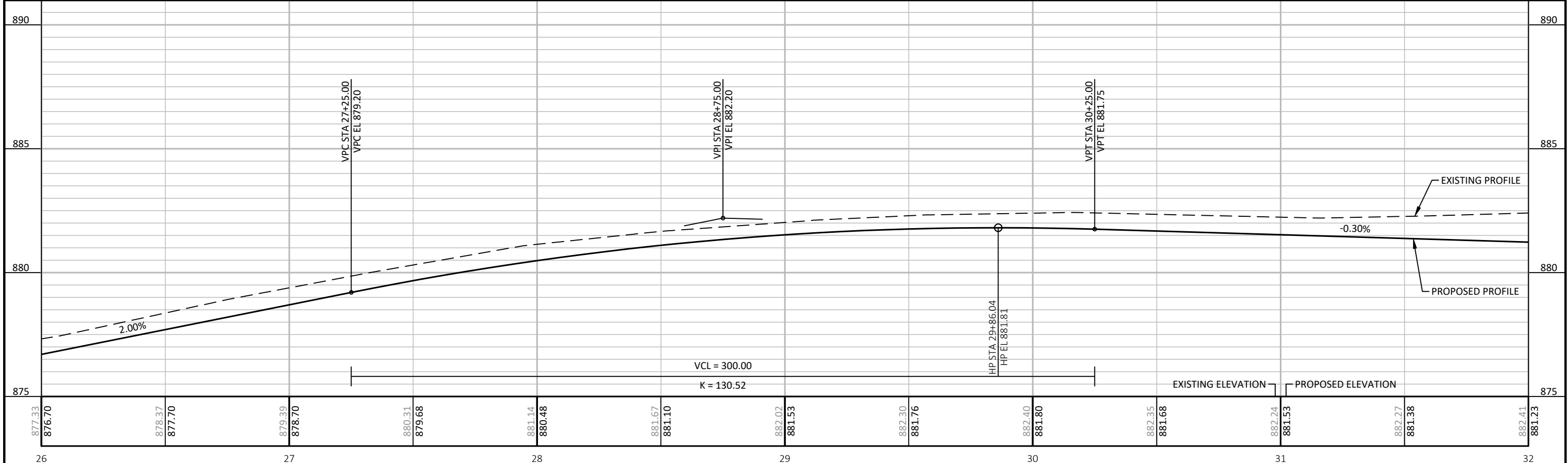


PROJECT NO: 7995-02-63	HWY: CTH F	COUNTY: EAU CLAIRE	PLAN AND PROFILE: CTH F / STATE ST	SHEET: E
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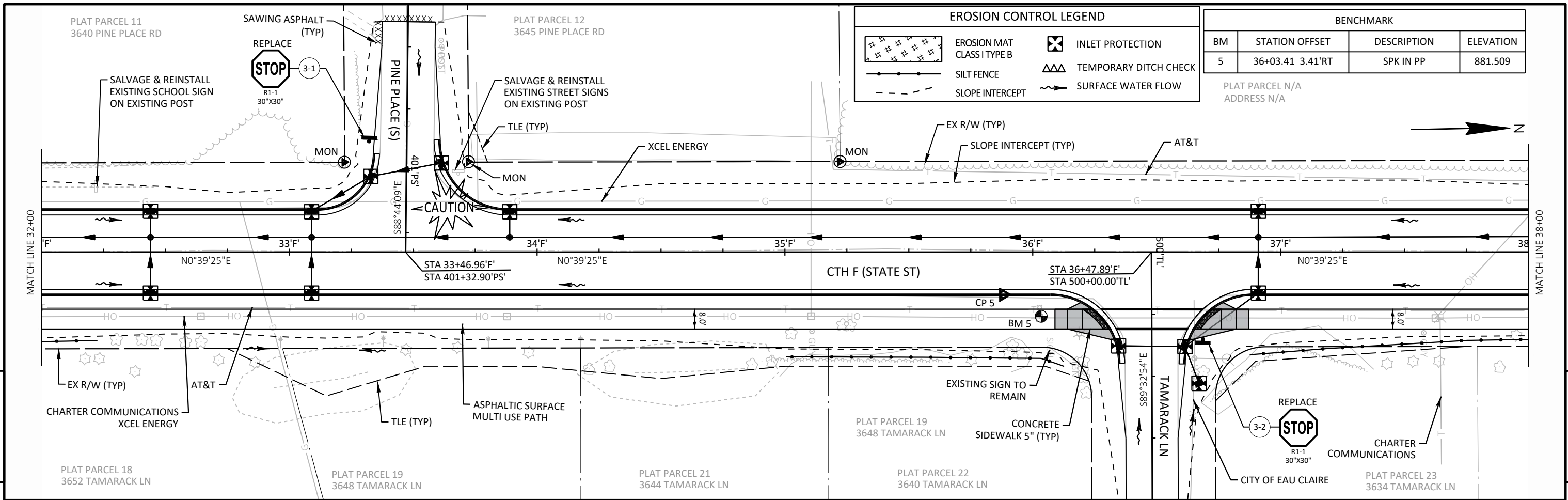


BENCHMARK			
BM	STATION OFFSET	DESCRIPTION	ELEVATION
3	28+63.53 46.63'LT	SPK IN PP	881.137
4	31+38.33 54.89'LT	RR SPK IN PP	880.414

EROSION CONTROL LEGEND	
	EROSION MAT CLASS I TYPE B
	SILT FENCE
	SLOPE INTERCEPT
	INLET PROTECTION
	TEMPORARY DITCH CHECK
	SURFACE WATER FLOW



PROJECT NO: 7995-02-63	HWY: CTH F	COUNTY: EAU CLAIRE	PLAN AND PROFILE: CTH F / STATE ST	SHEET E
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### EROSION CONTROL LEGEND

	EROSION MAT CLASS I TYPE B		INLET PROTECTION
	SILT FENCE		TEMPORARY DITCH CHECK
	SLOPE INTERCEPT		SURFACE WATER FLOW

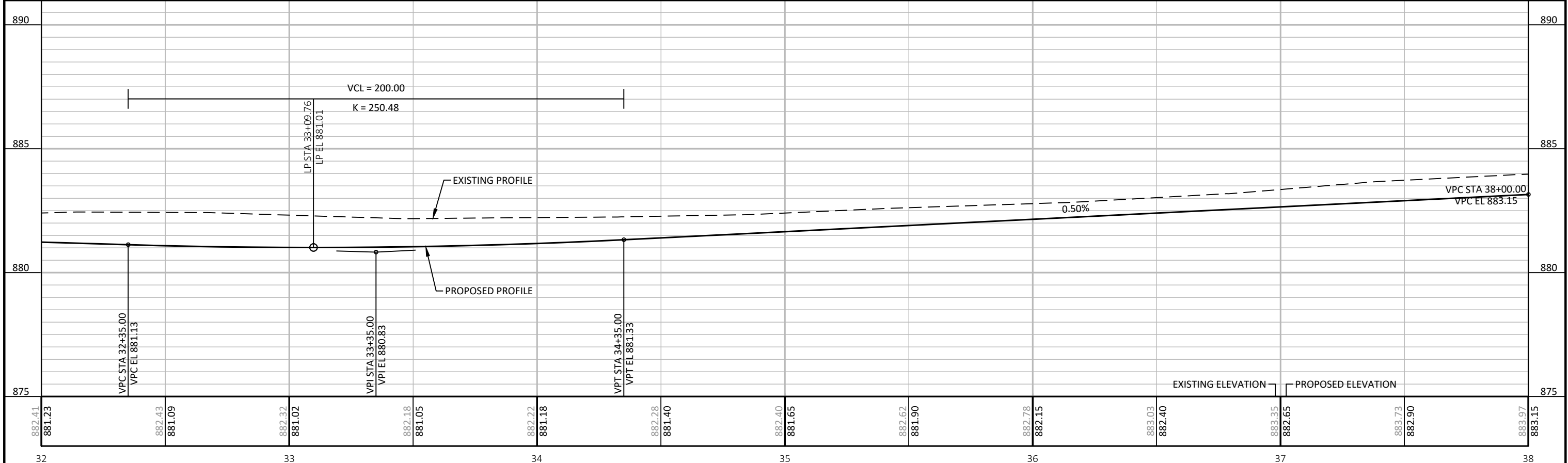
### BENCHMARK

BM	STATION OFFSET	DESCRIPTION	ELEVATION
5	36+03.41 3.41'RT	SPK IN PP	881.509

PLAT PARCEL N/A  
ADDRESS N/A

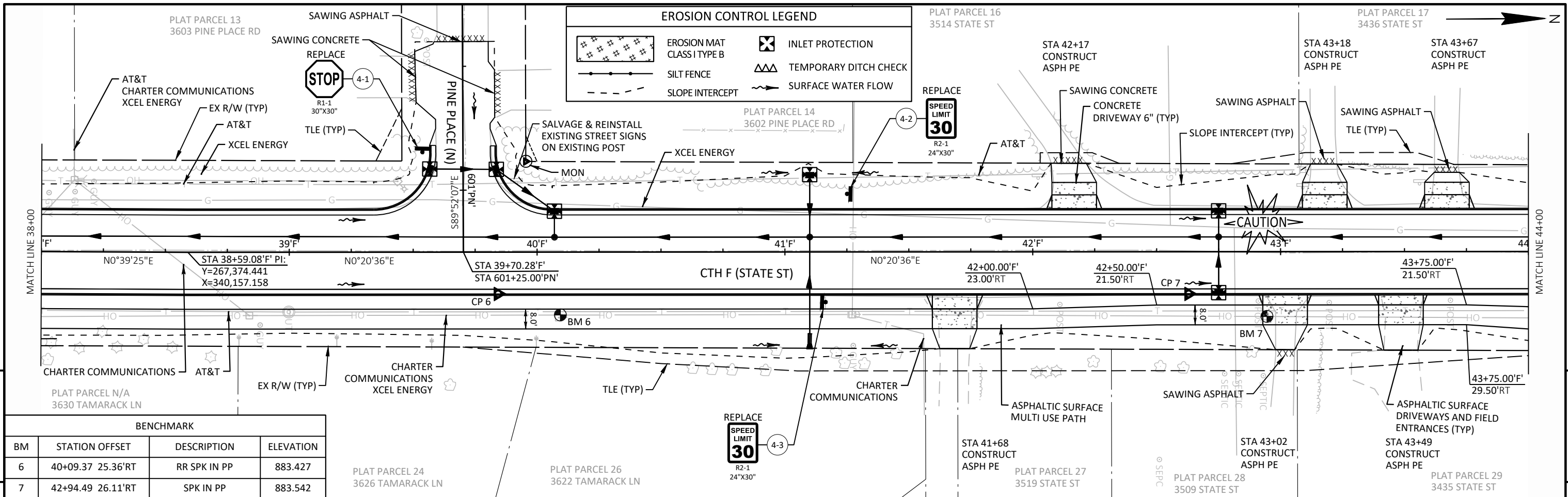
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5

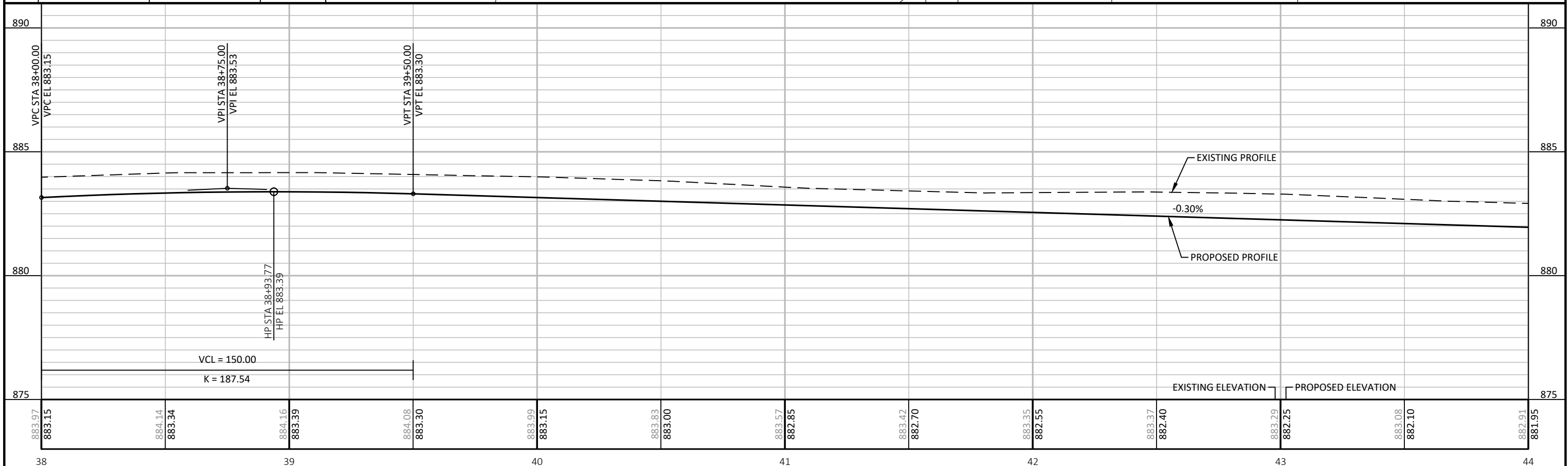


PROJECT NO: 7995-02-63	HWY: CTH F	COUNTY: EAU CLAIRE	PLAN AND PROFILE: CTH F / STATE ST	SHEET	<b>E</b>
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BENCHMARK			
BM	STATION OFFSET	DESCRIPTION	ELEVATION
6	40+09.37 25.36'RT	RR SPK IN PP	883.427
7	42+94.49 26.11'RT	SPK IN PP	883.542



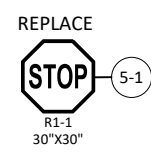
PROJECT NO: 7995-02-63	HWY: CTH F	COUNTY: EAU CLAIRE	PLAN AND PROFILE: CTH F / STATE ST	SHEET: E
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BENCHMARK			
BM	STATION OFFSET	DESCRIPTION	ELEVATION
8	46+69.07 27.41'RT	SPK IN PP	882.440

STA 45+61  
CONSTRUCT  
ASPH PE

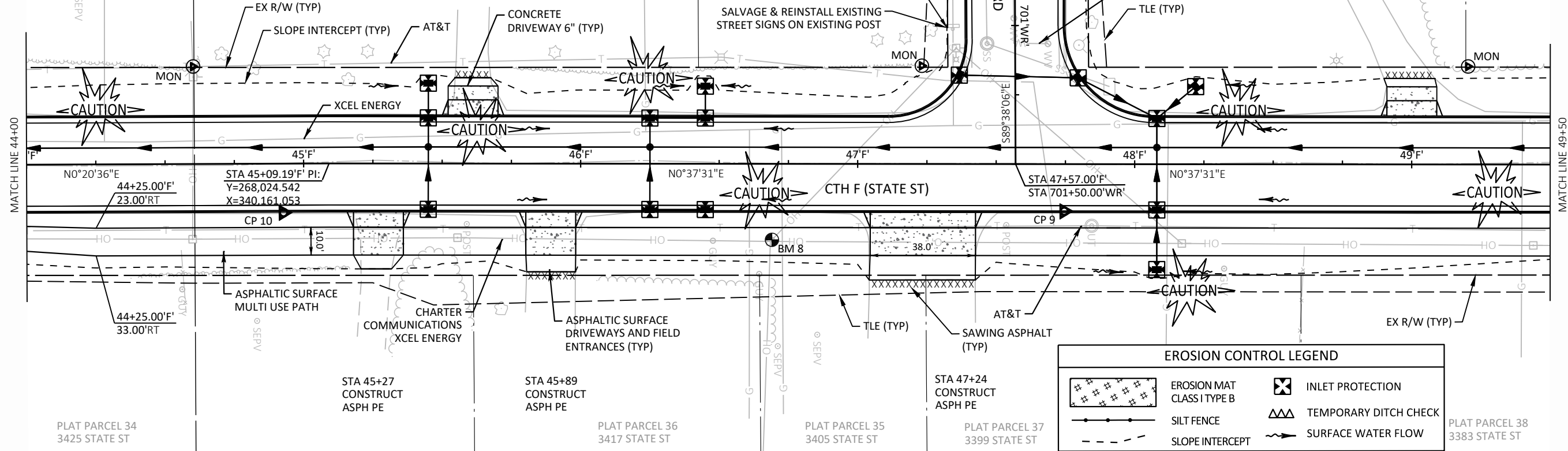
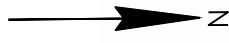
PLAT PARCEL N/A  
3416 STATE ST

PLAT PARCEL 31  
109 WESTOVER RD



PLAT PARCEL 32  
3390 STATE ST

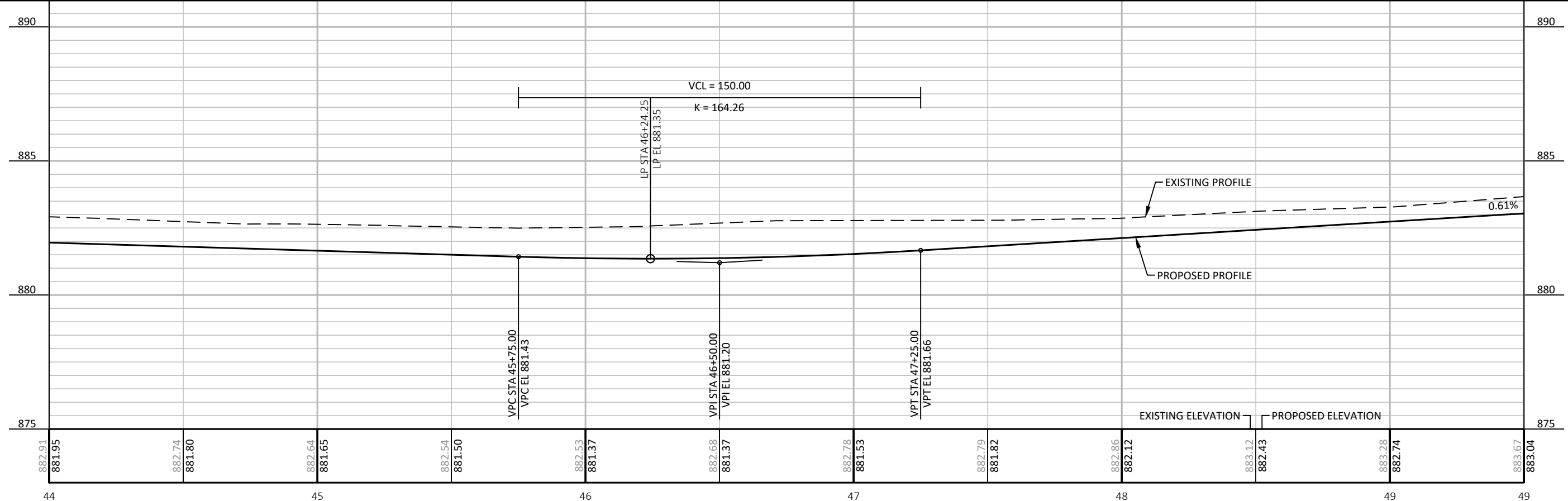
STA 49+00  
CONSTRUCT  
ASPH PE



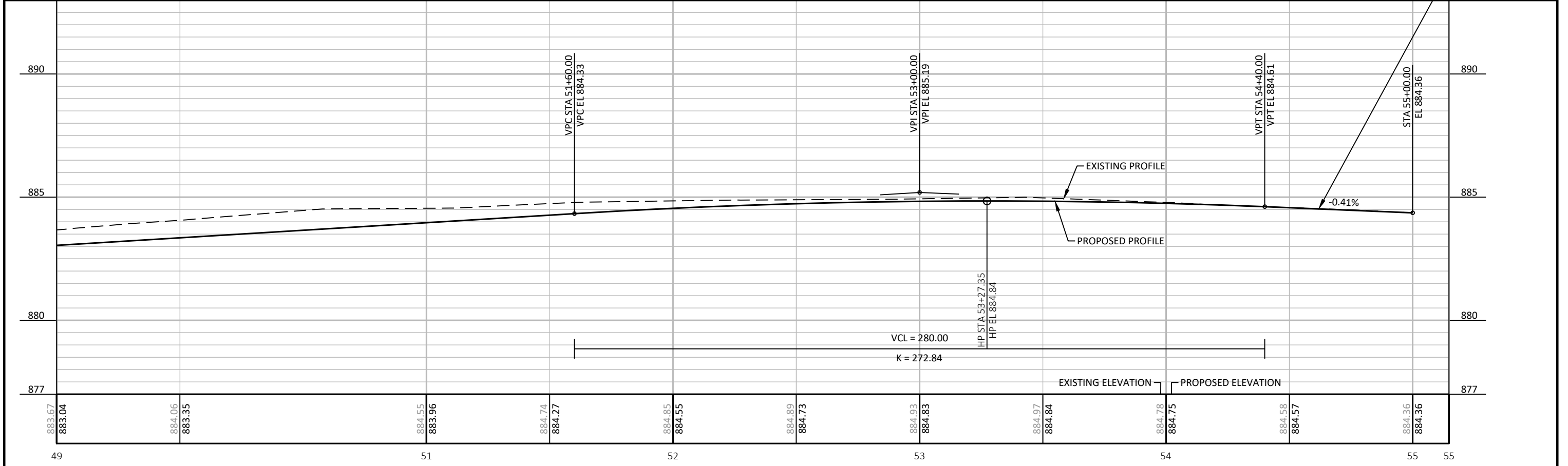
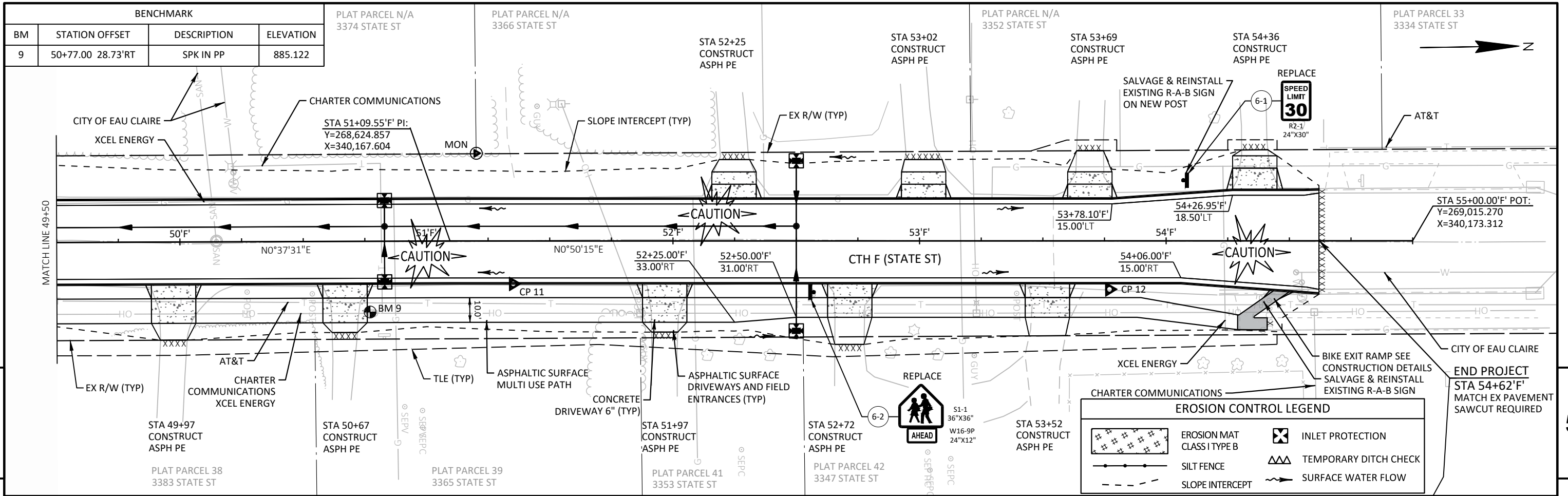
EROSION CONTROL LEGEND			
	EROSION MAT CLASS I TYPE B		INLET PROTECTION
	SILT FENCE		TEMPORARY DITCH CHECK
	SLOPE INTERCEPT		SURFACE WATER FLOW

5

5

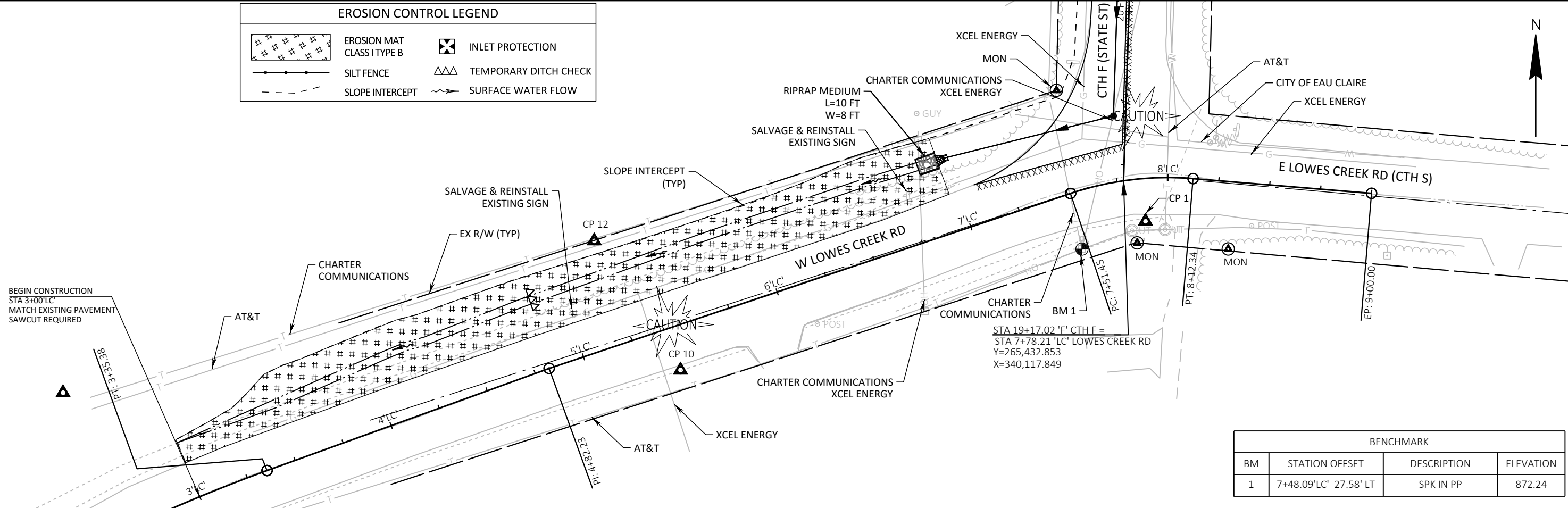


PROJECT NO: 7995-02-63	HWY: CTH F	COUNTY: EAU CLAIRE	PLAN AND PROFILE: CTH F / STATE ST	SHEET	E
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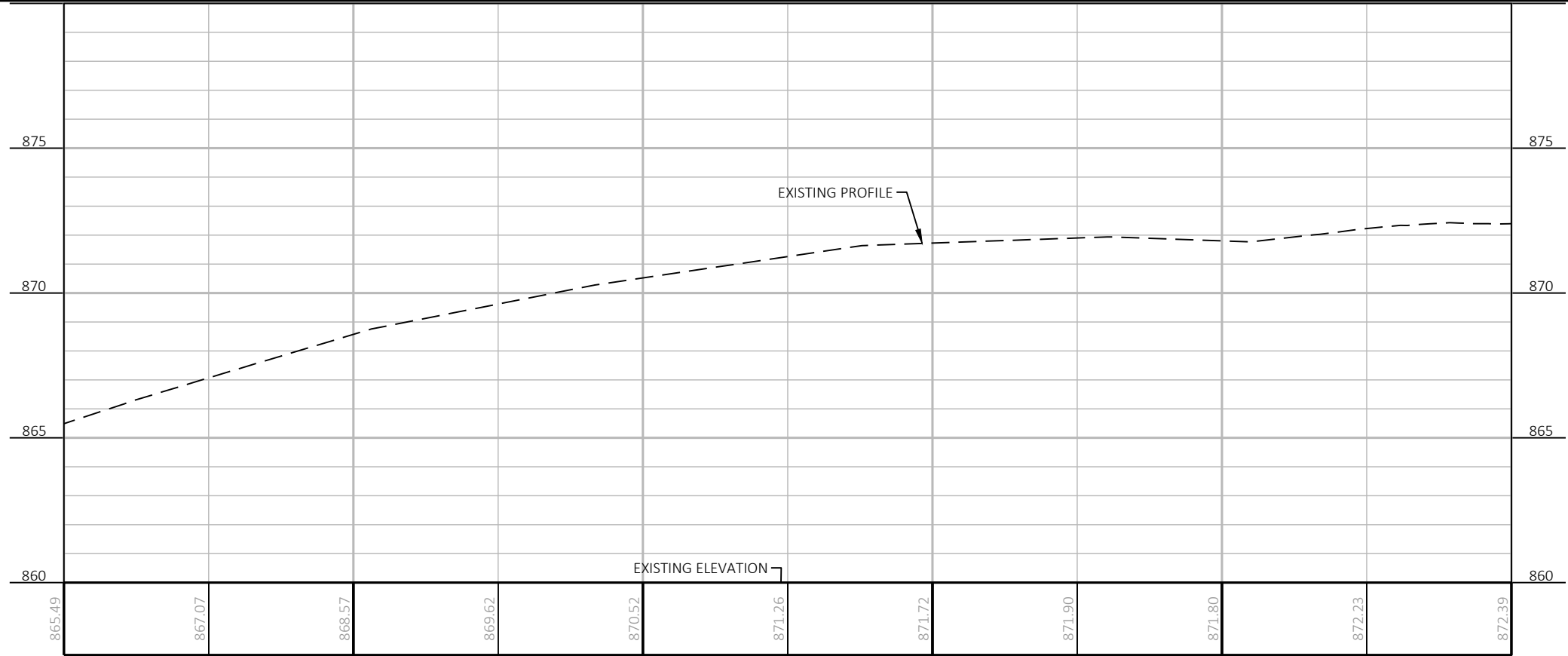


PROJECT NO: 7995-02-63	HWY: CTH F	COUNTY: EAU CLAIRE	PLAN AND PROFILE: CTH F / STATE ST	SHEET	E
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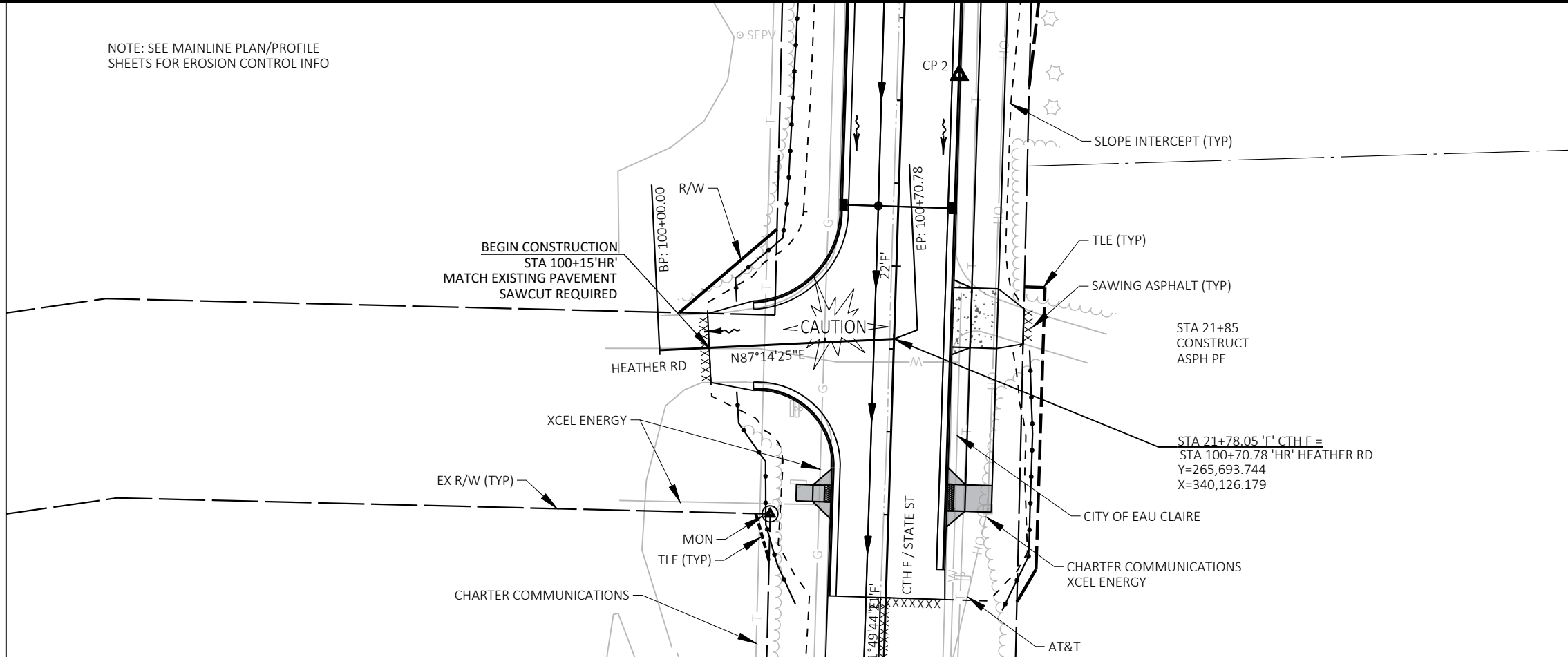
EROSION CONTROL LEGEND			
	EROSION MAT CLASS I TYPE B		INLET PROTECTION
	SILT FENCE		TEMPORARY DITCH CHECK
	SLOPE INTERCEPT		SURFACE WATER FLOW



BENCHMARK			
BM	STATION OFFSET	DESCRIPTION	ELEVATION
1	7+48.09' LC 27.58' LT	SPK IN PP	872.24

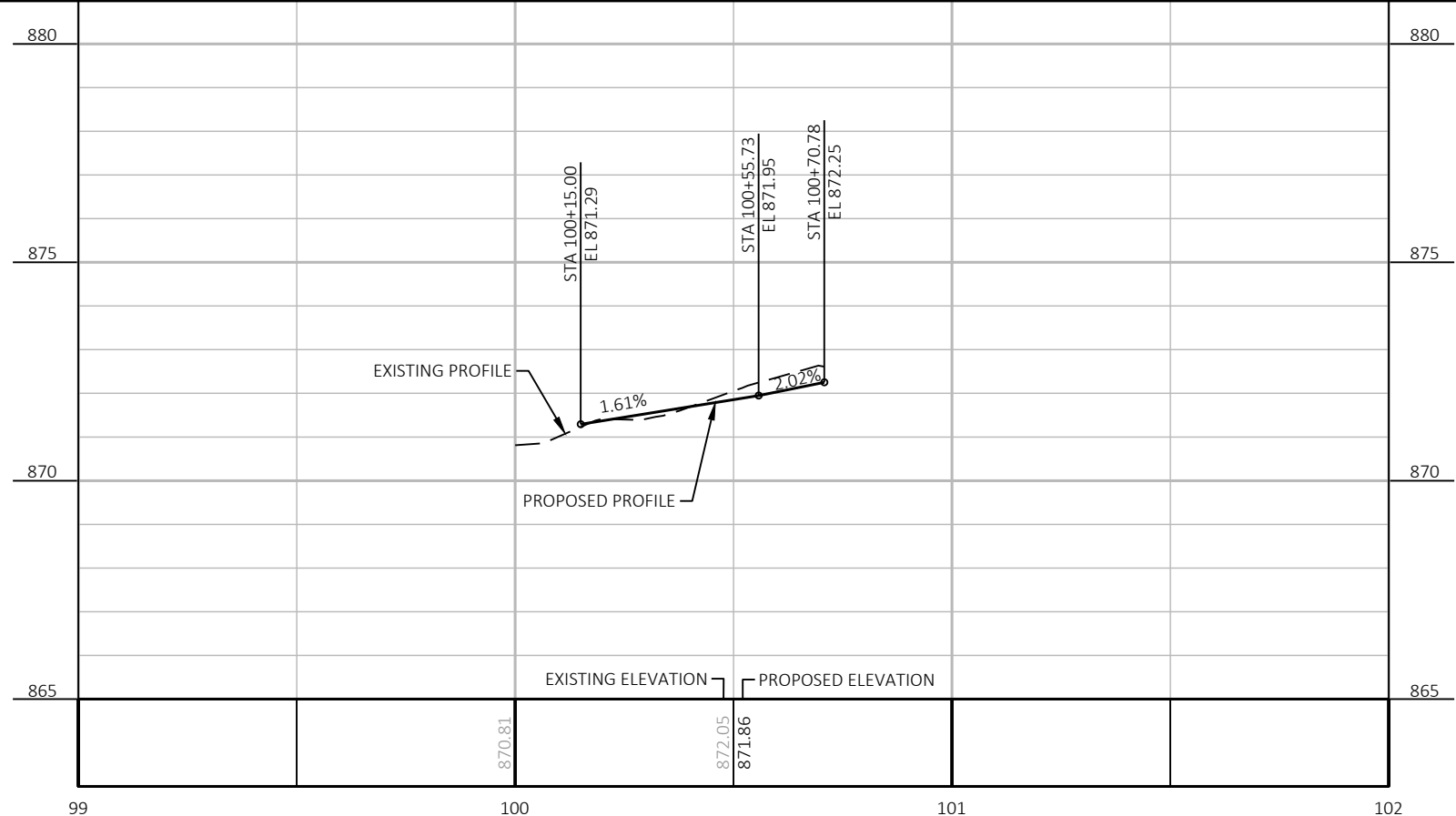


NOTE: SEE MAINLINE PLAN/PROFILE SHEETS FOR EROSION CONTROL INFO



5

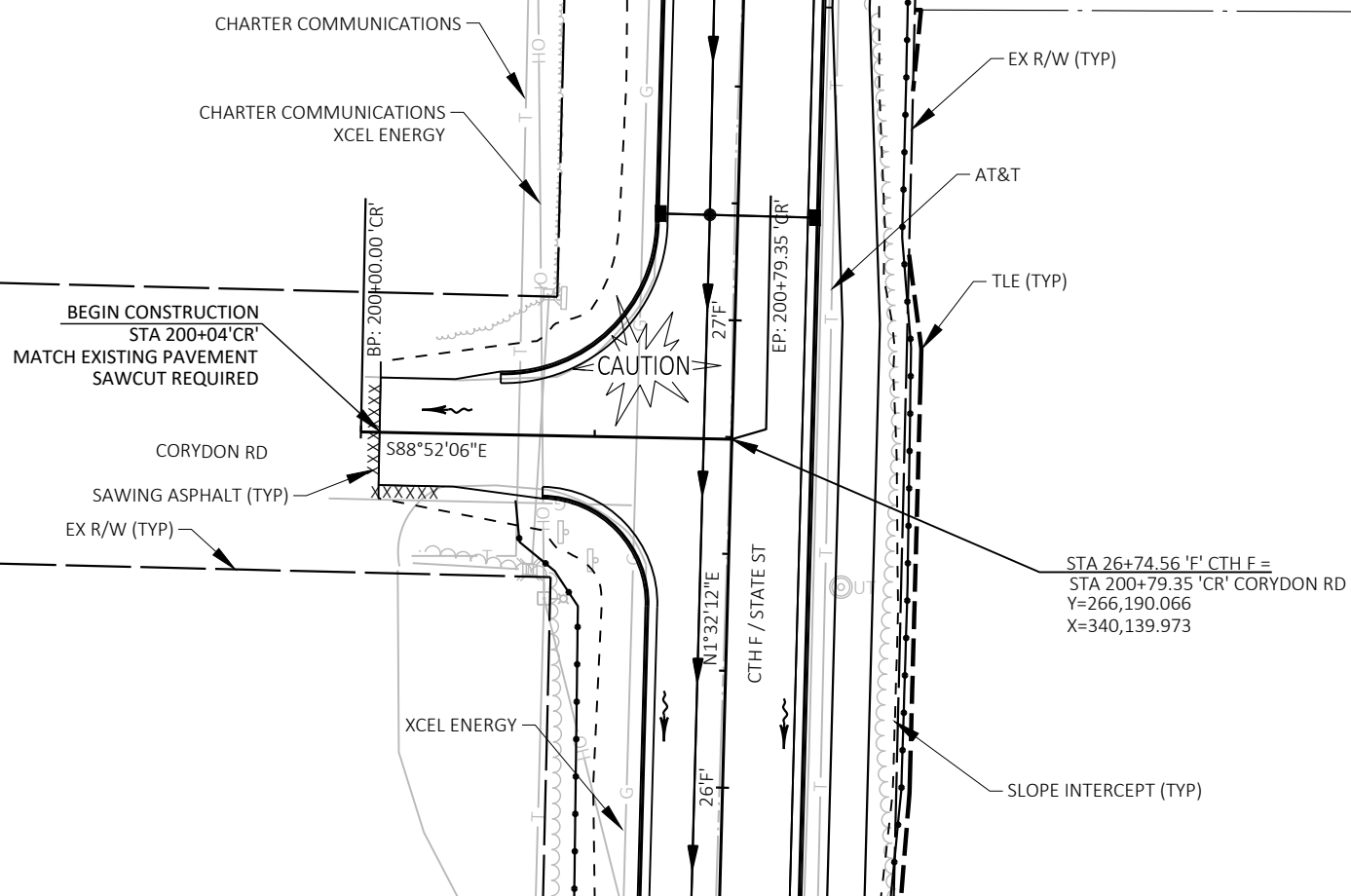
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PROJECT NO: 7995-02-63      HWY: CTH F      COUNTY: EAU CLAIRE      PLAN AND PROFILE: HEATHER ROAD      SHEET: E

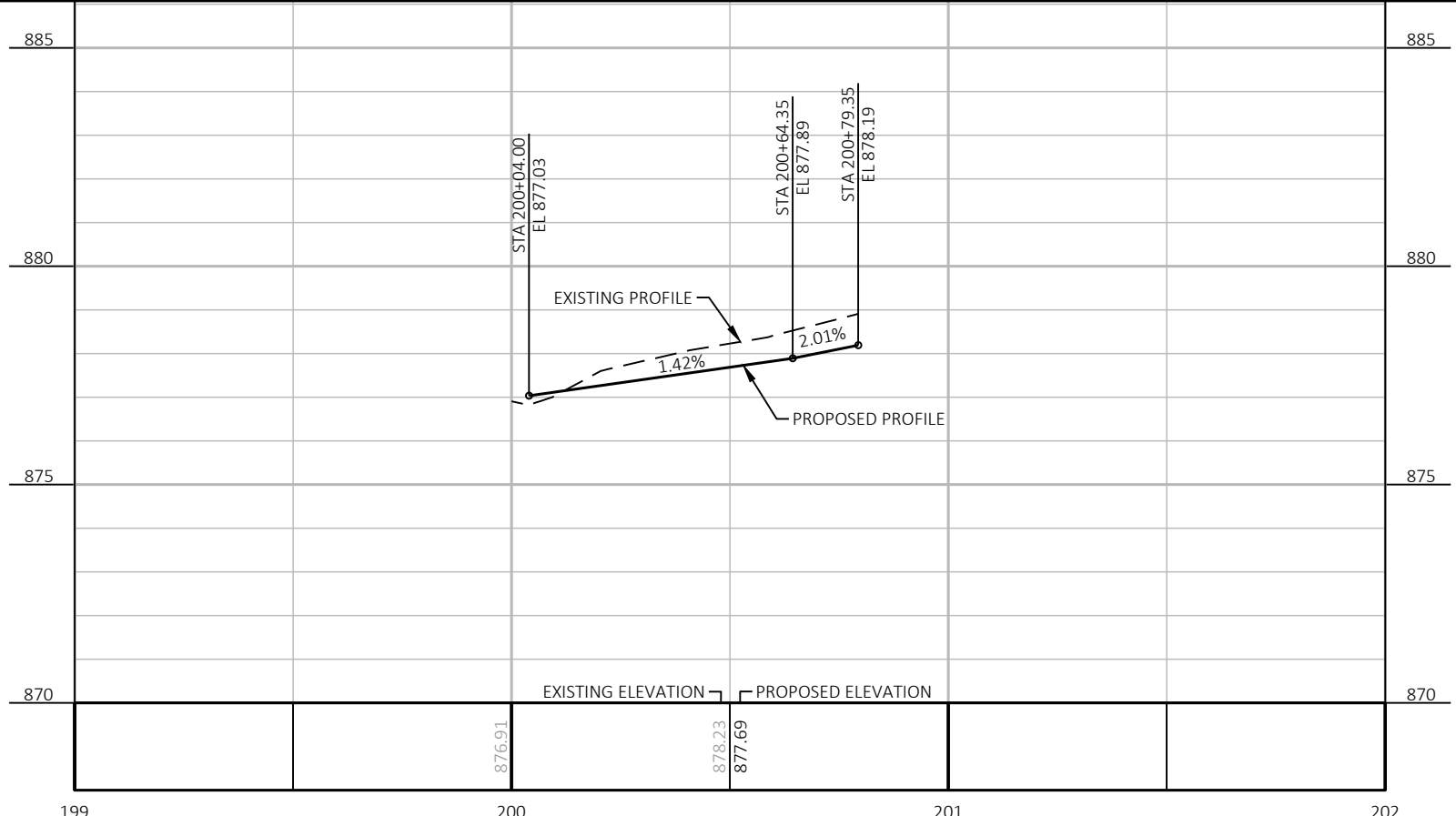
FILE NAME: \\SEHINC.COM\PANZURA\PZPROJECTS\AE\EAUCH\165167\5-FINAL-DSGN\CIVIL 3D-STATE ST\SHEETSPLAN\050102-PP.DWG      PLOT DATE: 1/18/2024 7:33 PM      PLOT BY: JUSTIN SHAVLIK      PLOT NAME:      PLOT SCALE: 1 IN:40 FT      WISDOT/CADD SHEET 44

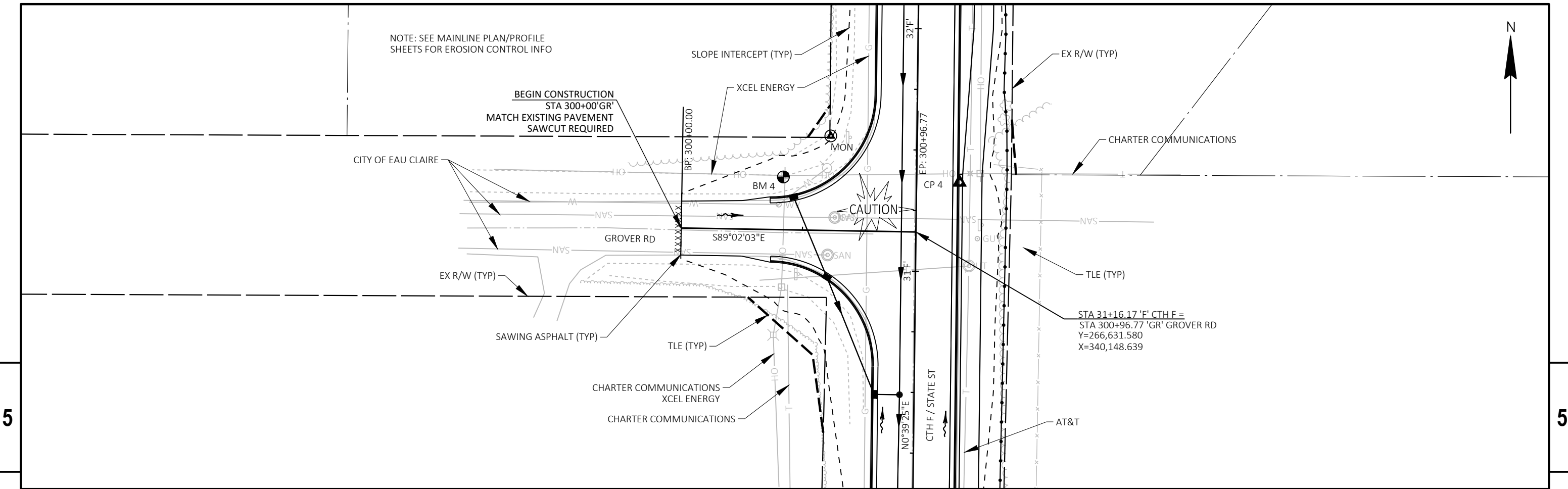
NOTE: SEE MAINLINE PLAN/PROFILE SHEETS FOR EROSION CONTROL INFO



5

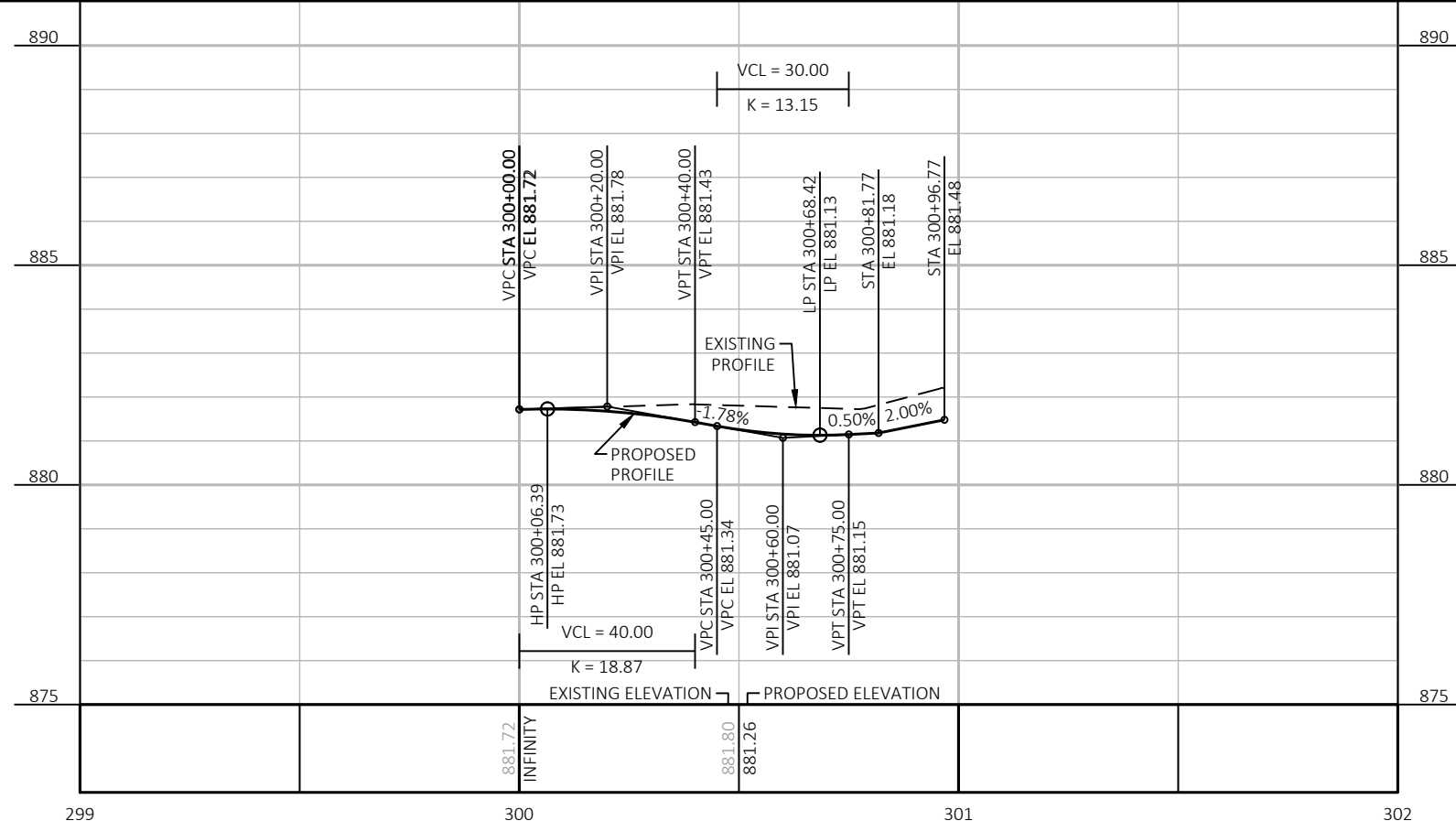
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5

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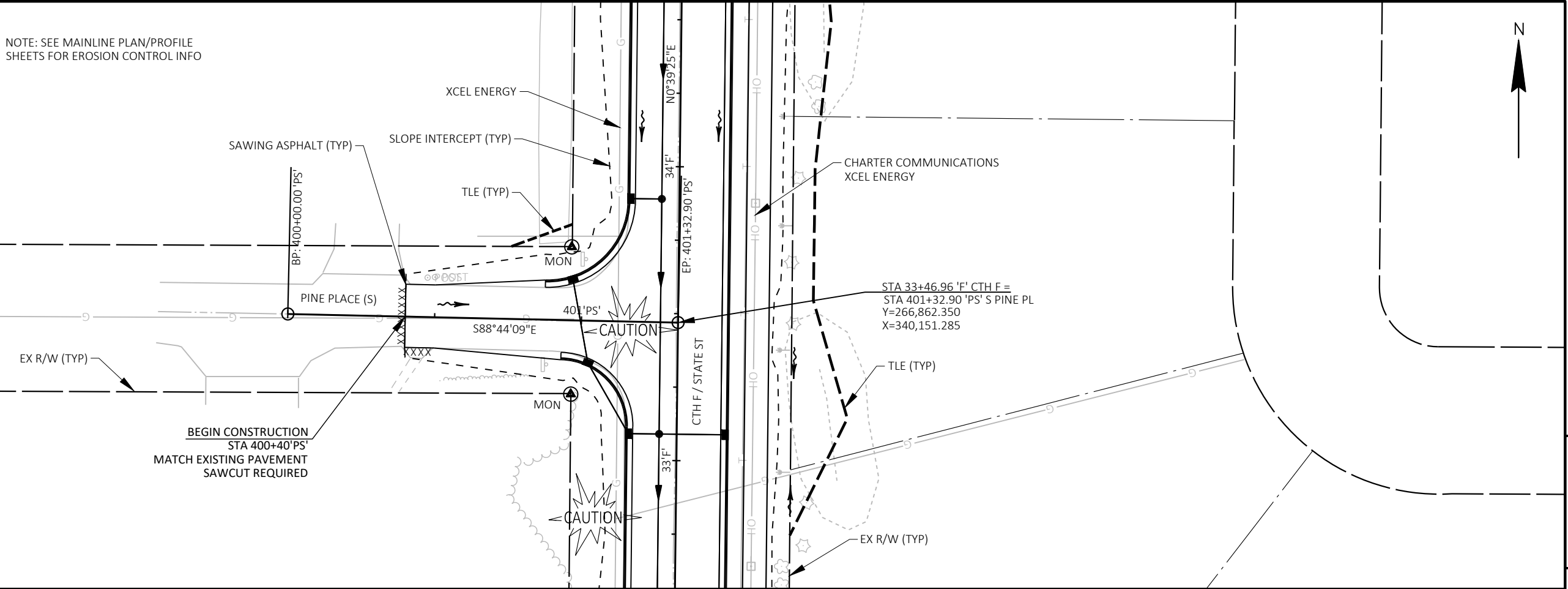
PROJECT NO: 7995-02-63      HWY: CTH F      COUNTY: EAU CLAIRE      PLAN AND PROFILE: GROVER ROAD      SHEET: E

NOTE: SEE MAINLINE PLAN/PROFILE SHEETS FOR EROSION CONTROL INFO

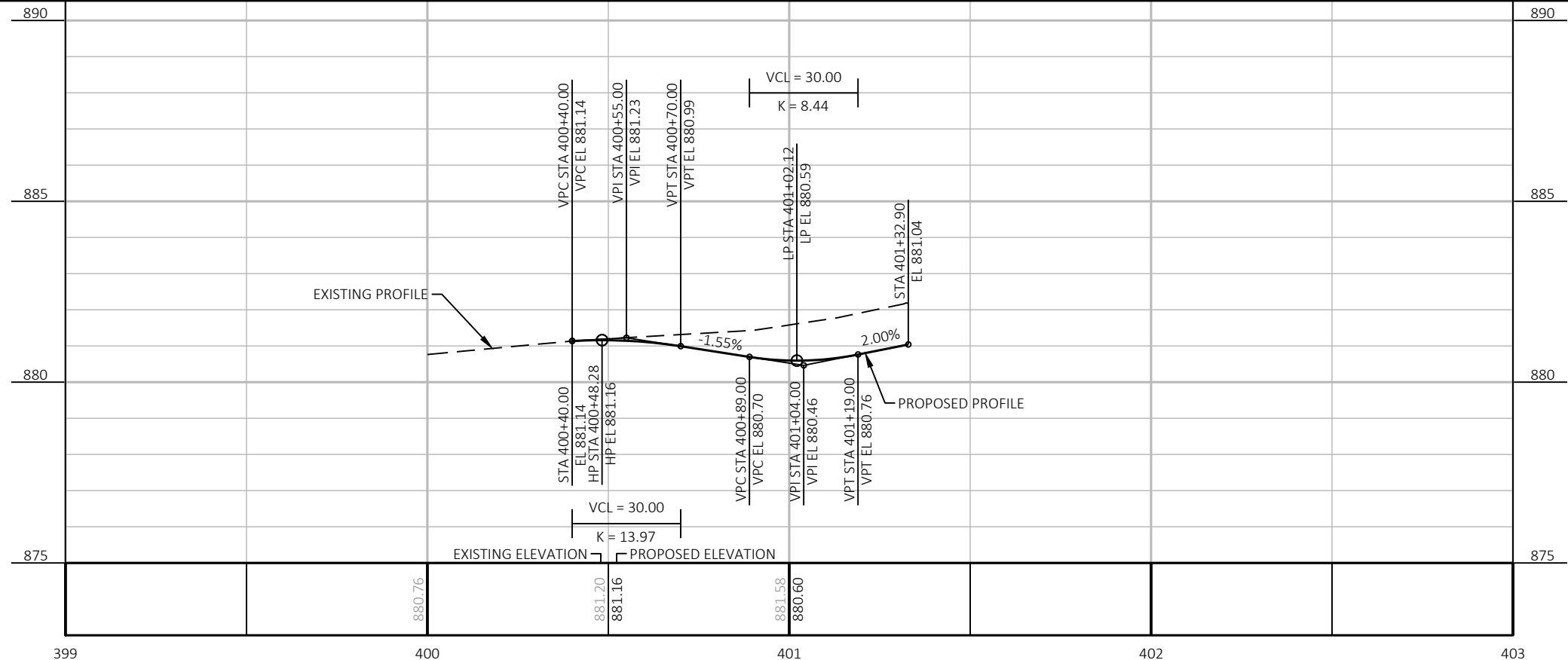


5

5



BEGIN CONSTRUCTION  
STA 400+40' PS'  
MATCH EXISTING PAVEMENT  
SAWCUT REQUIRED





NOTE: SEE MAINLINE PLAN/PROFILE SHEETS FOR EROSION CONTROL INFO



EX R/W (TYP)

XCEL ENERGY

CTH F / STATE ST

BP: 500+00.00 'TL'

36'F

CP 5

TLE (TYP)

S89°32'54"E

BEGIN CONSTRUCTION  
STA 501+00.00 'TL'  
MATCH EXISTING PAVEMENT  
SAWCUT REQUIRED

TAMARACK LN

SAWING ASPHALT (TYP)

STA 36+47.89 'F' CTH F =  
STA 500+00.00 'TL' TAMARACK LN  
Y=267,163.263  
X=340,154.737

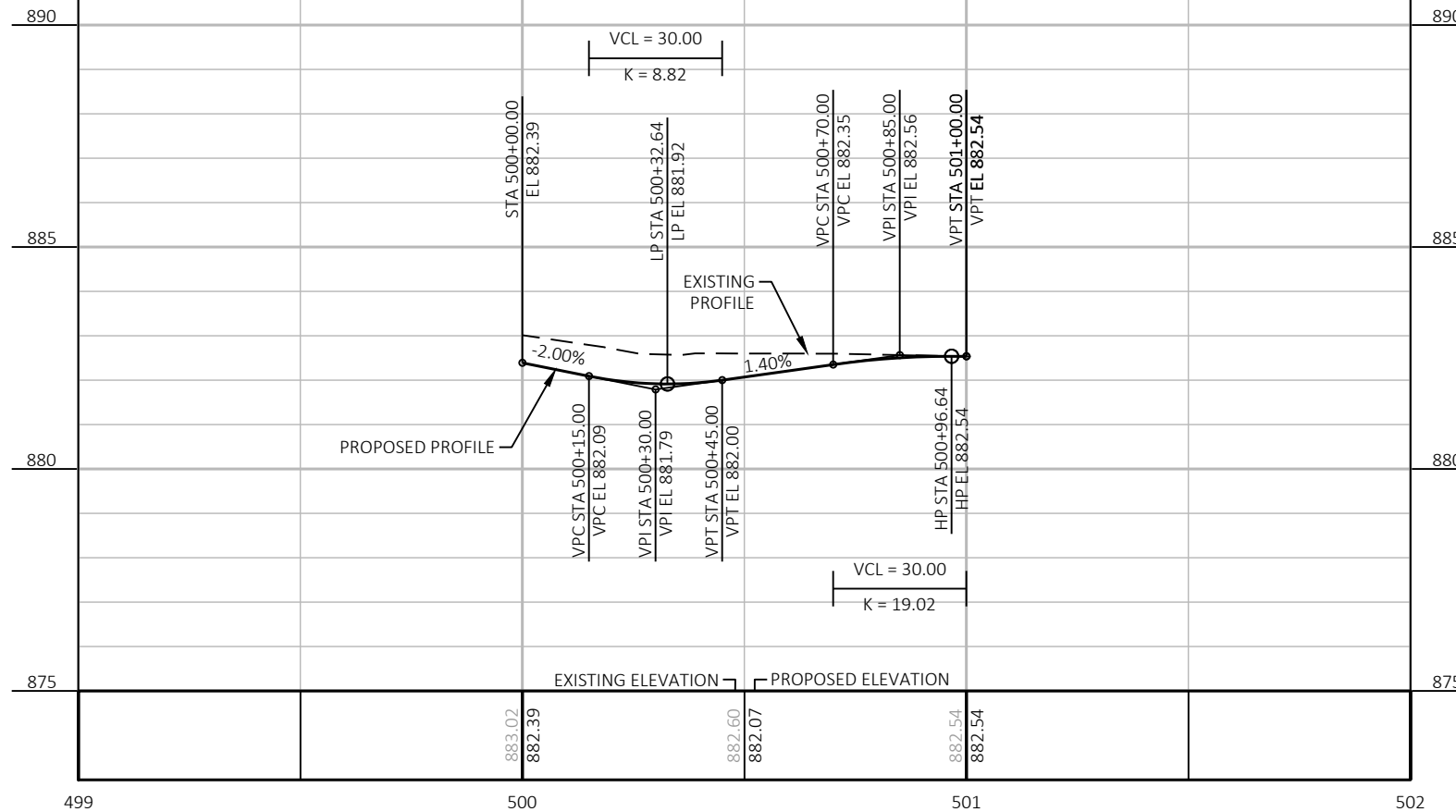
EX R/W (TYP)

EXISTING SIGN TO REMAIN

AT&T

SLOPE INTERCEPT (TYP)

CHARTER COMMUNICATIONS  
XCEL ENERGY



5

5

PROJECT NO: 7995-02-63

HWY: CTH F

COUNTY: EAU CLAIRE

PLAN AND PROFILE: TAMARACK LANE

SHEET

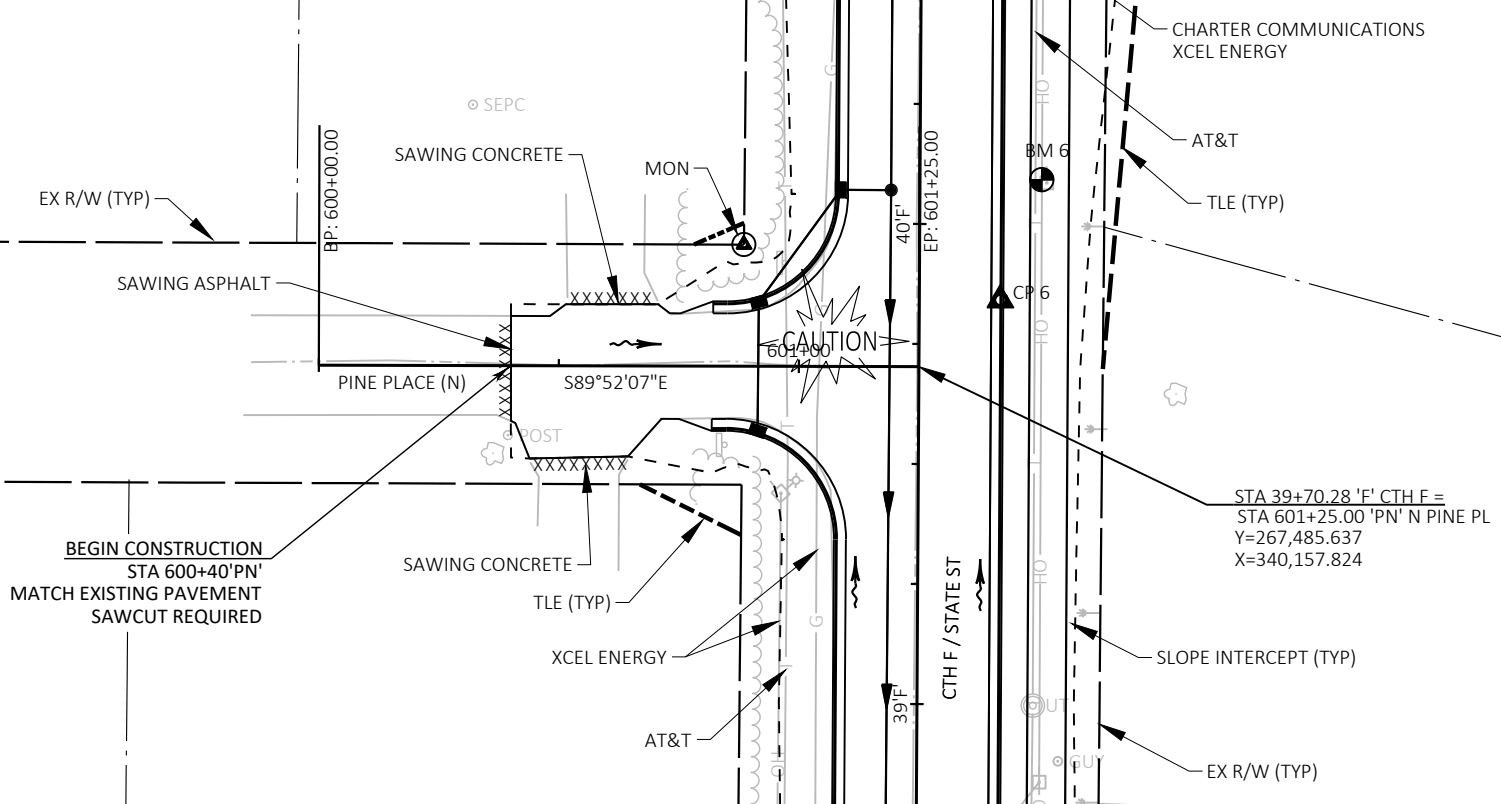
E

NOTE: SEE MAINLINE PLAN/PROFILE SHEETS FOR EROSION CONTROL INFO

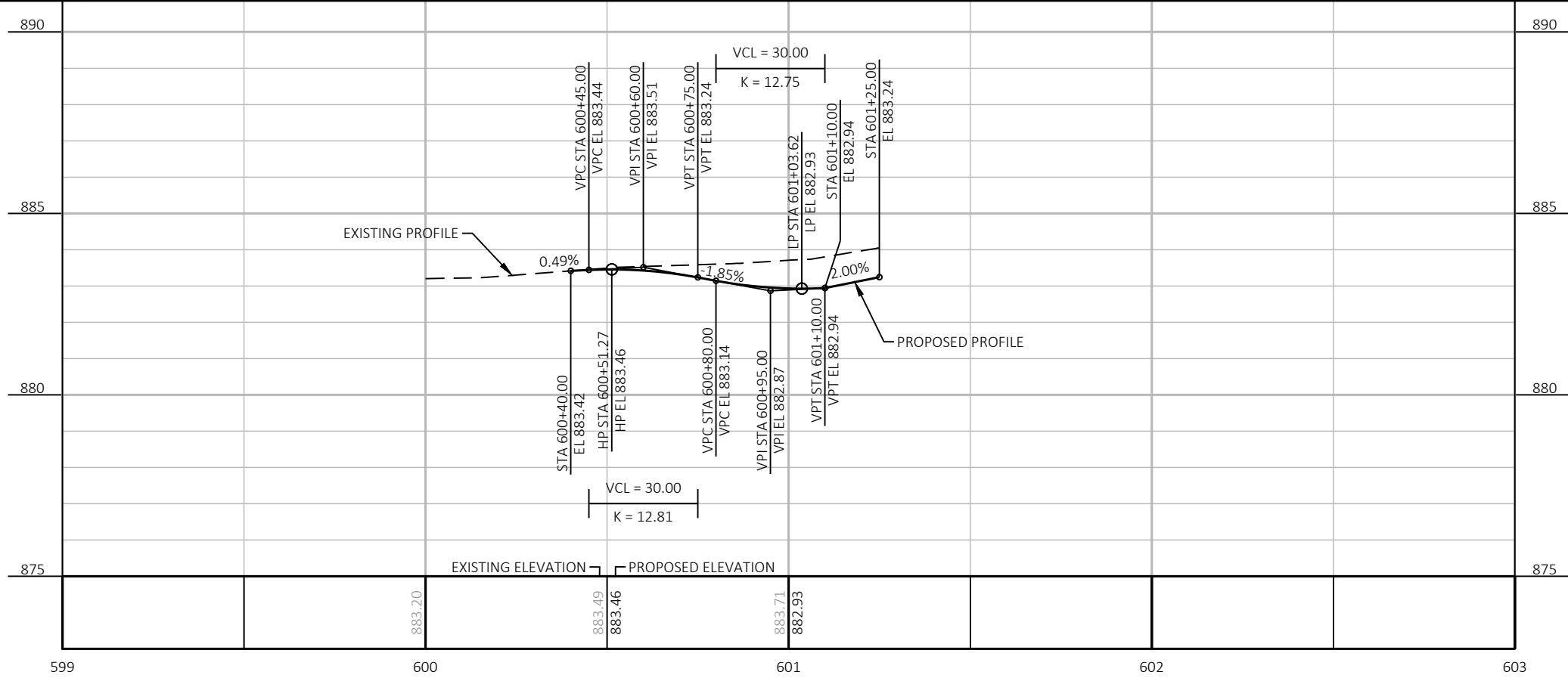


5

5



BEGIN CONSTRUCTION  
STA 600+40'PN'  
MATCH EXISTING PAVEMENT  
SAWCUT REQUIRED

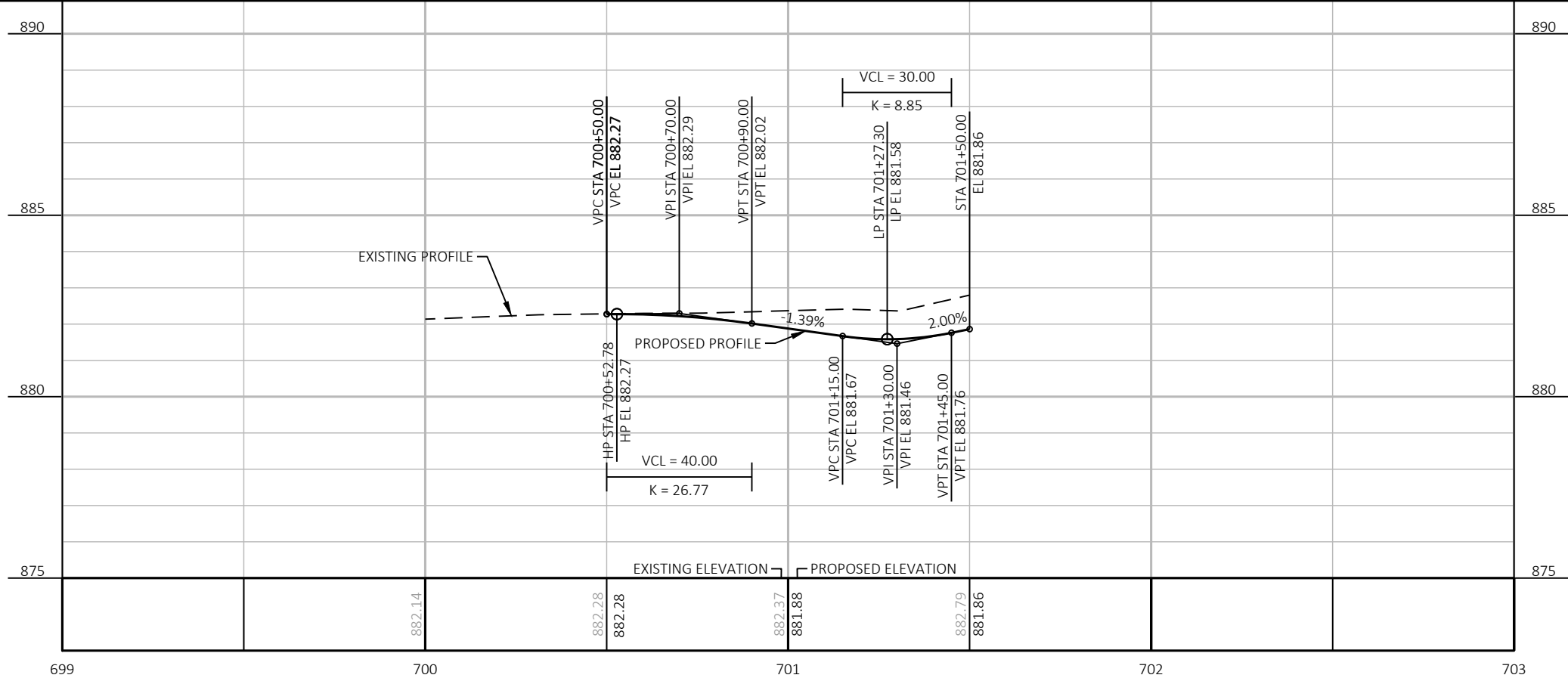
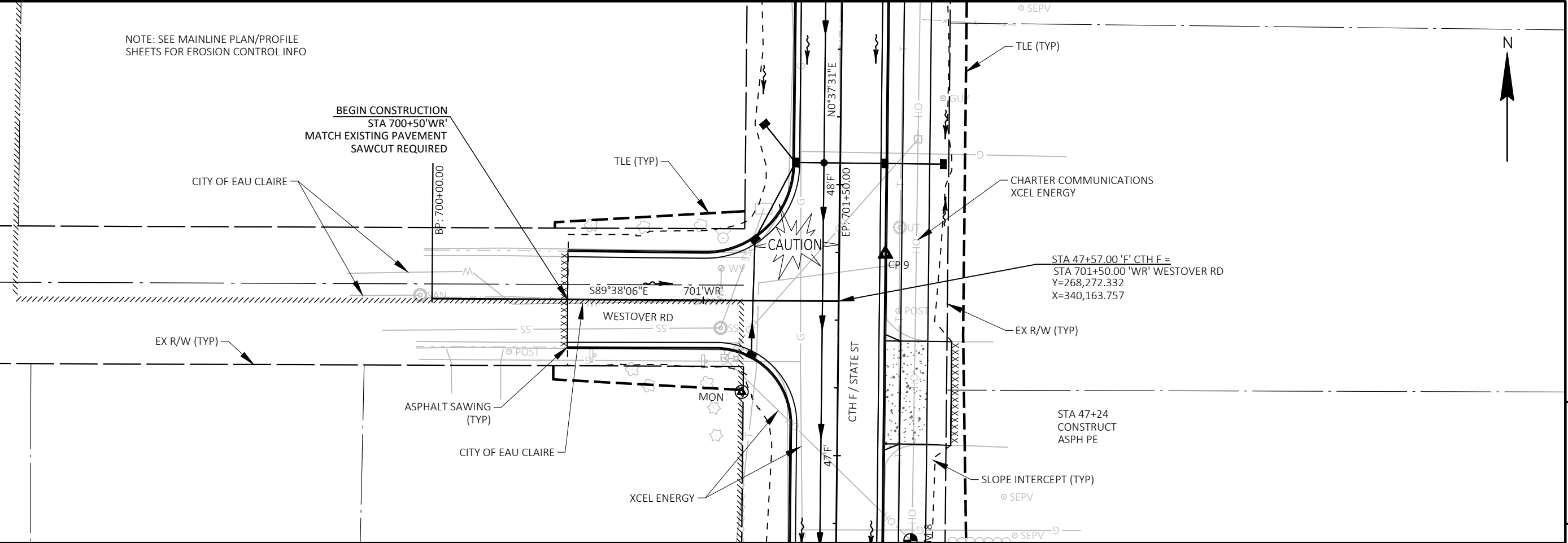


PROJECT NO: 7995-02-63	HWY: CTH F	COUNTY: EAU CLAIRE	PLAN AND PROFILE: PINE PLACE (NORTH)	SHEET E
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NOTE: SEE MAINLINE PLAN/PROFILE SHEETS FOR EROSION CONTROL INFO



BEGIN CONSTRUCTION  
STA 700+50'WR'  
MATCH EXISTING PAVEMENT  
SAWCUT REQUIRED



PROJECT NO: 7995-02-63

HWY: CTH F

COUNTY: EAU CLAIRE

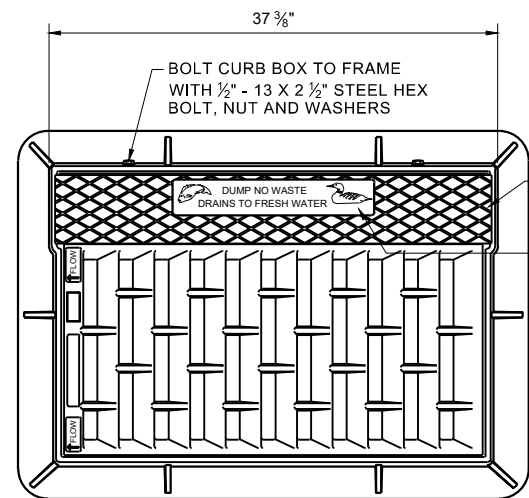
PLAN AND PROFILE: WESTOVER ROAD

SHEET

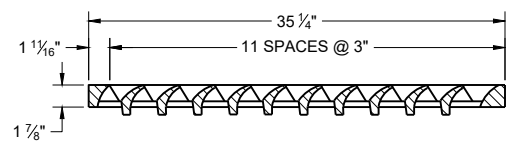
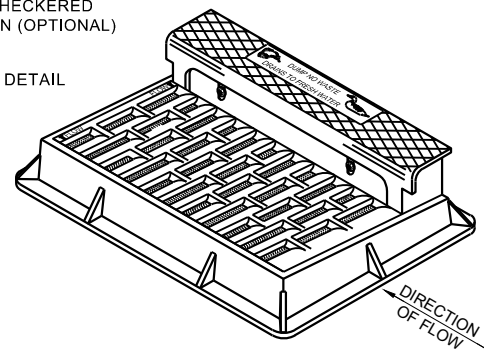
E

## Standard Detail Drawing List

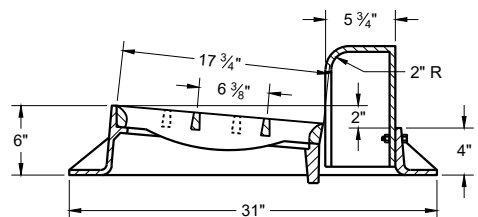
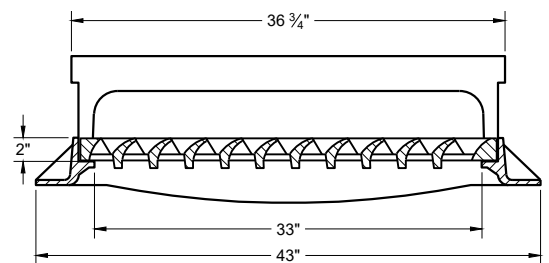
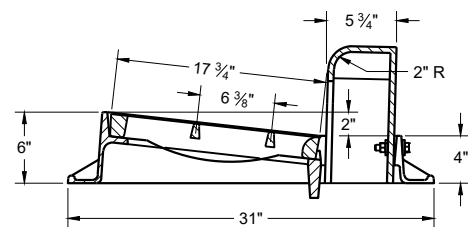
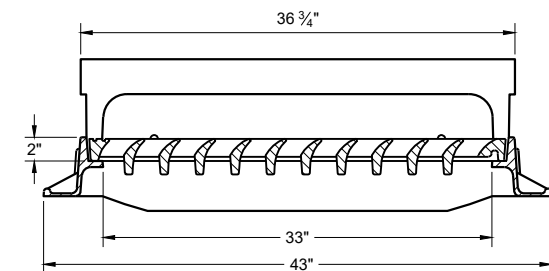
08A05-20A	INLET COVERS TYPE A, H, A-S, H-S & Z
08A05-20B	INLET COVERS TYPE B, B-A, C, MS, MS-A, & WM
08A05-20D	INLET COVER TYPE BW, MANHOLE COVERS, TYPE K, J, J-S, L & M
08A08-02	CATCH BASINS 3-FT, 4-FT, 5-FT AND 6-FT DIAMETER
08A09-02	CATCH BASINS 2X3-FT AND 2.5X3-FT
08B09-03	MANHOLES 3-FT, 4-FT, 5-FT, 6-FT, 7-FT, 8-FT, 9-FT, 10-FT DIAMETER
08C06-02	INLETS 3-FT AND 4-FT DIAMETER
08C07-02	INLETS 2X2-FT, 2X2.5-FT, 2X3-FT AND 2.5X3-FT
08C08-02	INLETS MEDIAN 1 AND 2 GRATE
08D01-23A	CONCRETE CURB & GUTTER
08D01-23B	CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS
08D05-21A	CURB RAMPS TYPES 1 AND 1-A
08D05-21B	CURB RAMPS TYPES 2 AND 3
08D05-21C	CURB RAMPS TYPES 4A AND 4A1
08D05-21D	CURB RAMPS TYPE 4B AND 4B1
08D05-21E	CURB RAMPS TYPES 5, 6, 7A, 7B & 8
08D05-21F	CURB RAMPS RADIAL DETECTABLE WARNING FIELD APPLICATIONS
08D05-21G	CURB RAMPS RECTANGULAR AND RADIAL DETECTABLE WARNING PLATES
08D18-04	DRIVEWAY AND SIDEWALK RAMPS TYPES X & Y
08D19-04	DRIVEWAY AND SIDEWALK RAMPS TYPE Z
08E08-03	TYPICAL INSTALLATIONS OF EROSION BALES / TEMPORARY DITCH CHECKS
08E09-06	SILT FENCE
08E10-02	INLET PROTECTION TYPE A, B, C AND D
08E14-01	TRACKING PAD
08F01-11	APRON ENDWALLS FOR CULVERT PIPE
08F04-08	JOINT TIES FOR CONCRETE PIPE AND CONCRETE COLLAR DETAIL
13C19-03	HMA LONGITUDINAL JOINTS
15A03-02A	FLEXIBLE MARKER POST FOR CULVERT END
15A03-02B	FLEXIBLE MARKER POST FOR CULVERT END
15C02-09A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-09C	DETOUR SIGNING FOR MAINLINE CLOSURES
15C03-05	BARRICADES AND SIGNS FOR SIDEROAD CLOSURES
15C08-23A	PERMANENT LONGITUDINAL PAVEMENT MARKINGS
15C11-10B	CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS
15C33-04	STOP LINE AND CROSSWALK PAVEMENT MARKING
15C35-06A	PAVEMENT MARKING (INTERSECTIONS)
15D28-04	TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY



NOTE: EITHER CASTING IS ACCEPTABLE



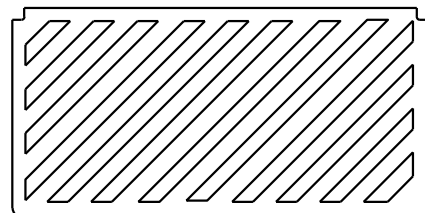
NOTE: CURB BOX HEIGHT ADJUSTABLE 6" - 9"



**TYPE "H"**

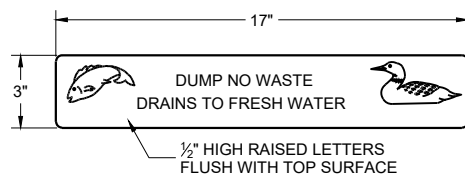
NOTE: EITHER CASTING IS ACCEPTABLE

1 1/8" DIAGONAL BARS WITH 1 5/8" OPENINGS



**SPECIAL GRATE FOR TYPE "H" COVER**

(MEASURES 35" X 17 3/4" X 2")  
(NOTED AS TYPE H-S ON DRAINAGE TABLE)



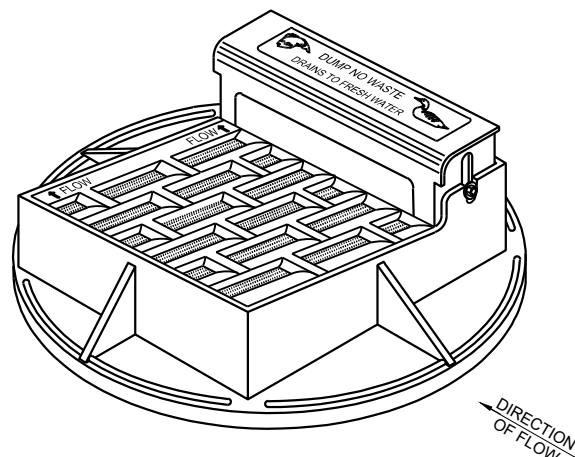
**LOGO DETAIL**

**GENERAL NOTES**

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

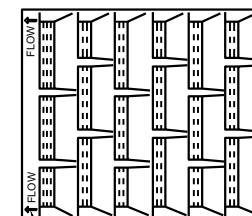
DETAIL DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR CATCH BASIN, MANHOLE AND INLET COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ROUND FRAMES AND COVERS SHALL HAVE CONTINUOUSLY MACHINED BEARING SURFACES TO PREVENT ROCKING AND RATTLING.

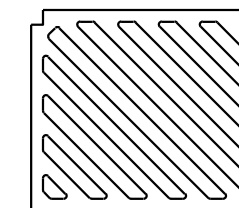


NOTE: CURB BOX HEIGHT ADJUSTABLE 6" - 9"

NOTE: EITHER CASTING IS ACCEPTABLE

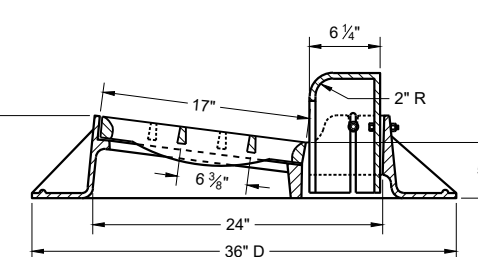
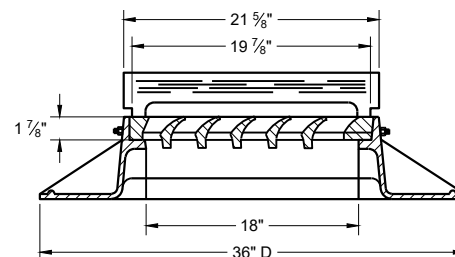
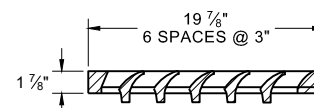


1" DIAGONAL BARS WITH 1 1/2" OPENINGS

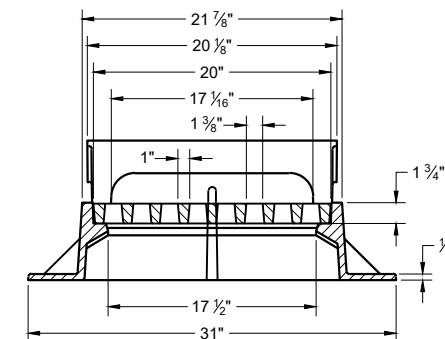
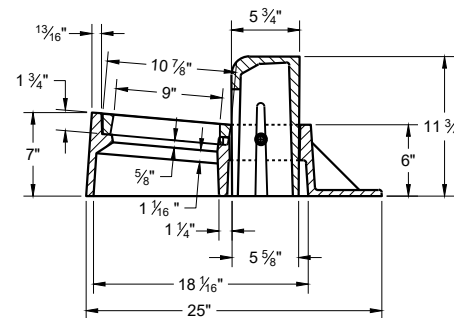


**SPECIAL GRATE FOR TYPE "A" COVER**

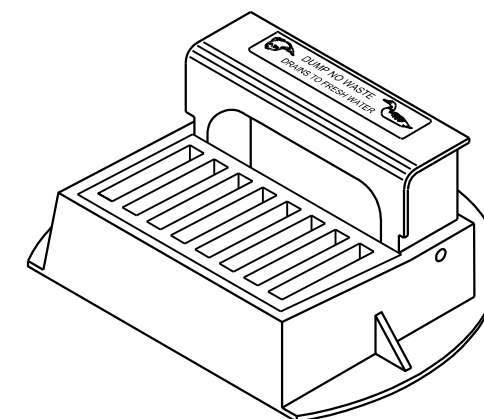
(MEASURES 19 3/4" X 17" X 1 7/8")  
(NOTED AS TYPE A-S ON DRAINAGE TABLE)



**TYPE "A"**



**TYPE "Z"**

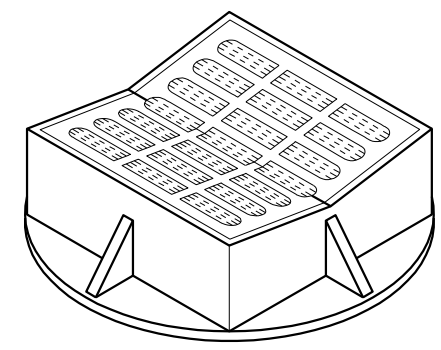
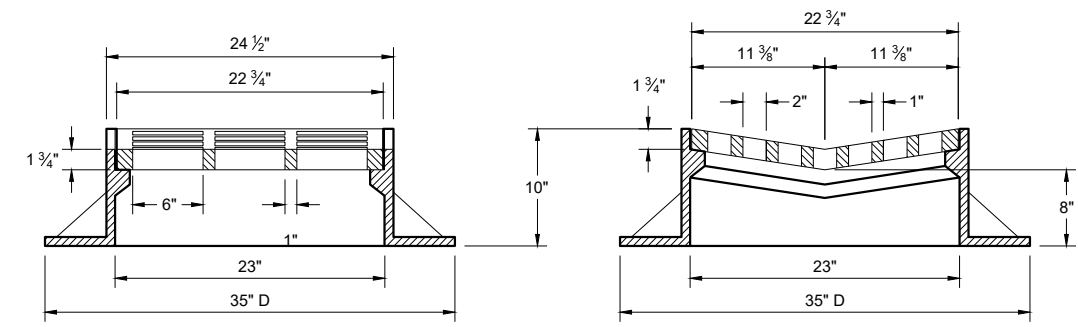


**INLET COVERS TYPES A, H, A-S, H-S AND Z**

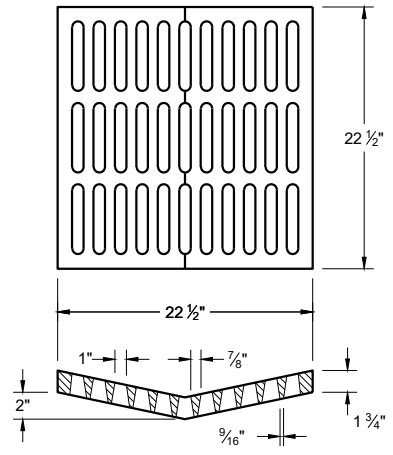
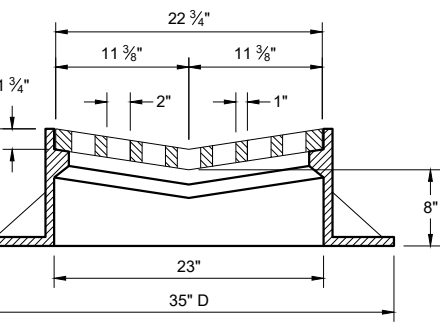
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
July 2023 DATE /S/ Rodney Taylor  
ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR

FHWA

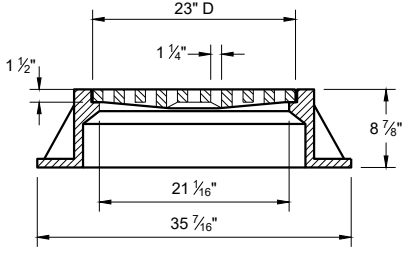
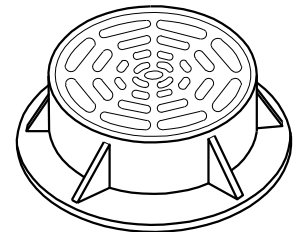
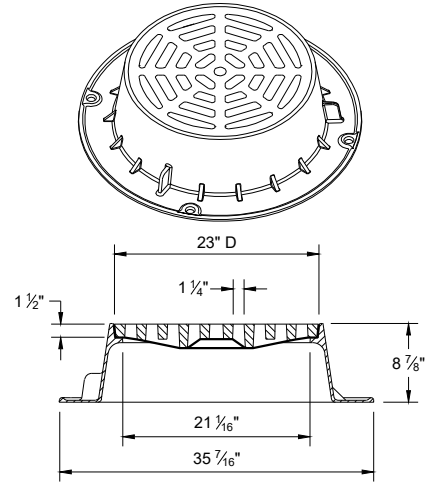


**TYPE "B"**



**ALTERNATIVE GRATE FOR TYPE "B" COVER**

USE WHERE PEDESTRIAN OR BICYCLE TRAFFIC IS POSSIBLE  
**NOTED AS TYPE B - A ON THE DRAINAGE TABLE**



**TYPE "C"**

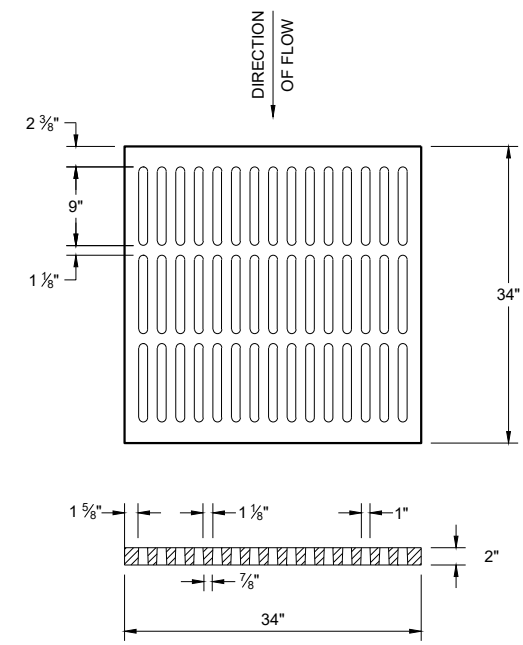
NOTE: EITHER CASTING IS ACCEPTABLE

**GENERAL NOTES**

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

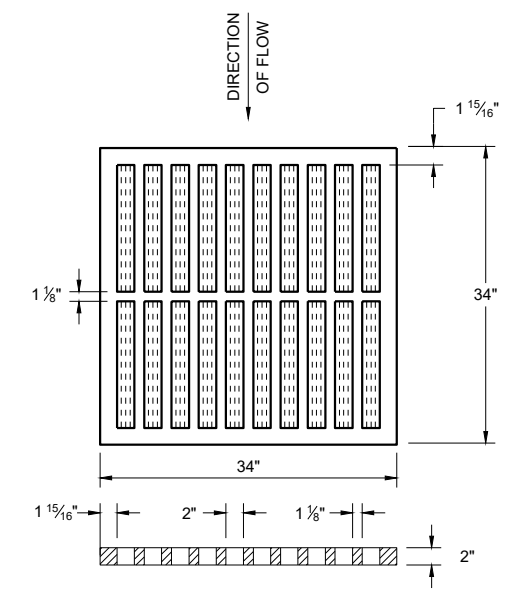
DETAIL DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR CATCH BASIN, MANHOLE AND INLET COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ROUND FRAMES AND COVERS SHALL HAVE CONTINUOUSLY MACHINED BEARING SURFACES TO PREVENT ROCKING AND RATTLING.



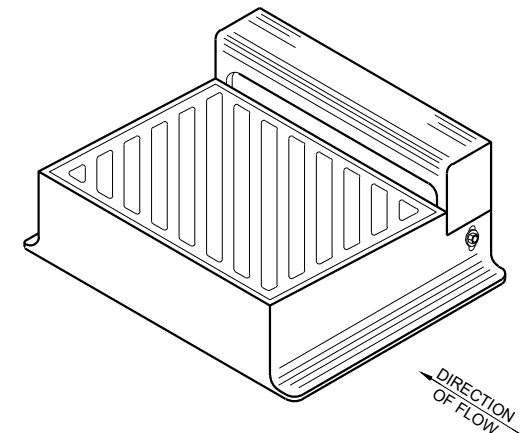
**ALTERNATIVE TYPE "MS"**

USE WHERE PEDESTRIAN OR BICYCLE TRAFFIC IS PERMITTED  
**NOTED AS TYPE MS-A ON THE DRAINAGE TABLE**

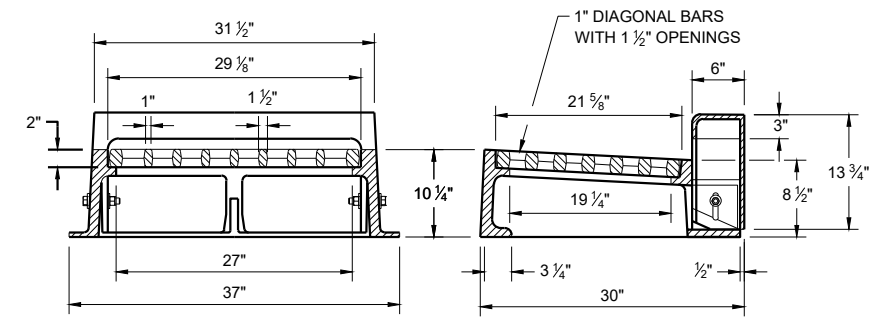


**TYPE "MS"**

USE ON FREEWAYS AND EXPRESSWAYS  
**NOTED AS TYPE MS ON THE DRAINAGE TABLE**



DIAGONAL SLOTS SHALL BE ORIENTED TO THE DIRECTION OF FLOW AS ILLUSTRATED.  
 GRATES ARE MANUFACTURED TO BE REVERSIBLE.



**TYPE "WM"**

NOTE: CURB BOX HEIGHT ADJUSTABLE 6" - 9"

6

6

SDD 08A05-20b

SDD 08A05-20b

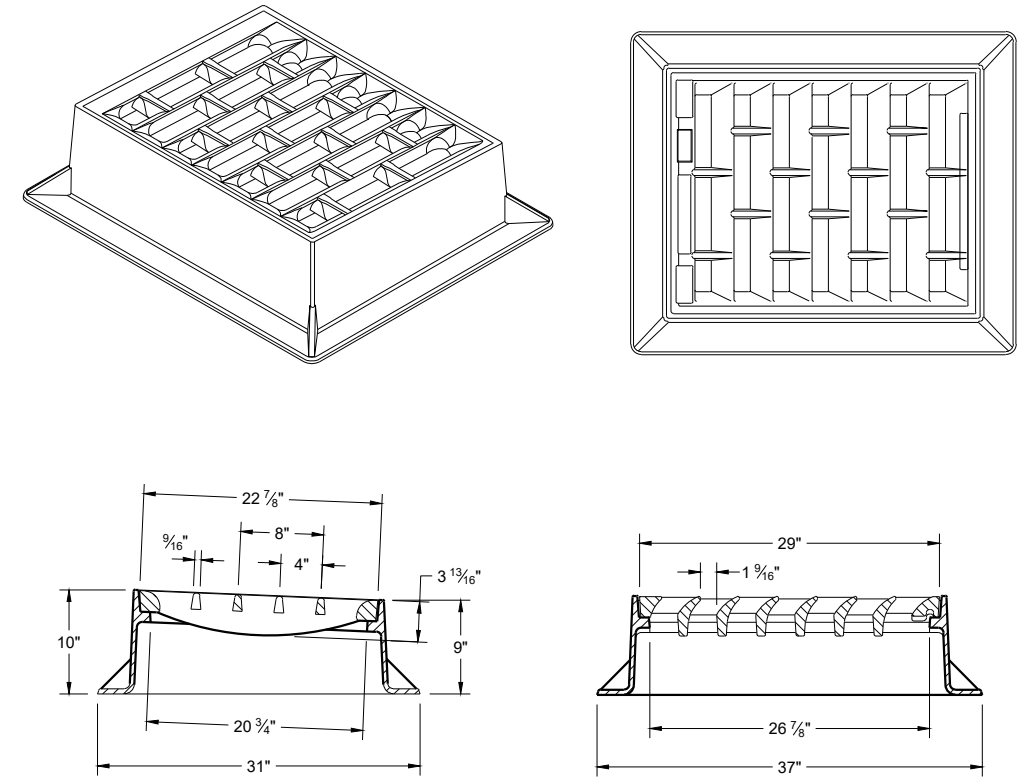
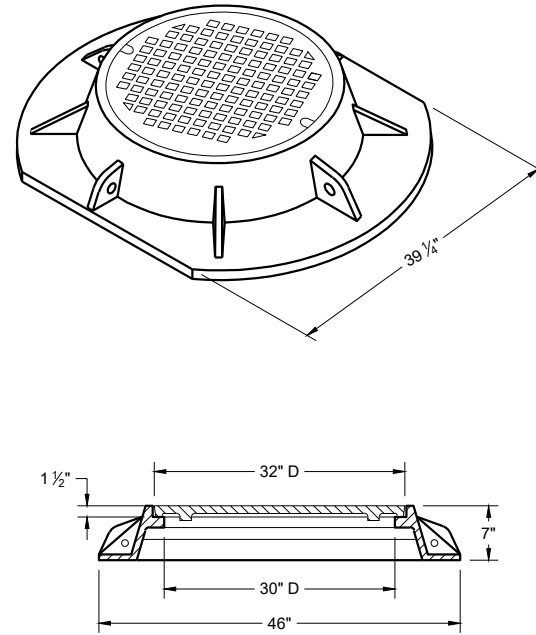
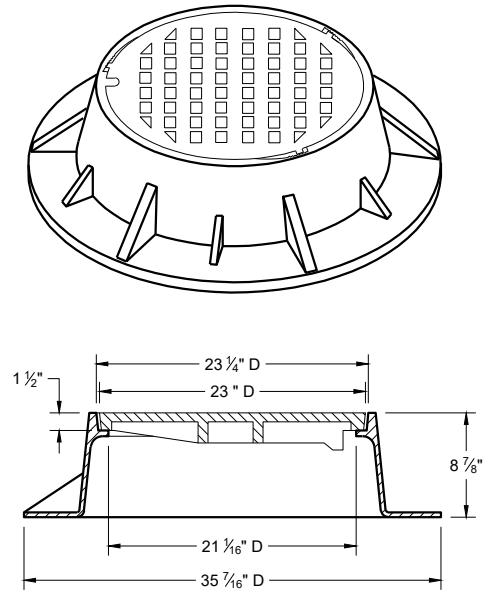
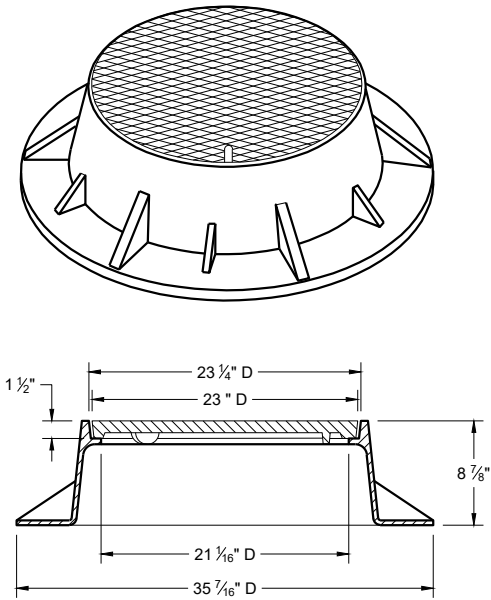
<b>INLET COVERS TYPES B, B-A, C, MS, MS-A AND WM</b>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED July 2023 DATE	/s/ Rodney Taylor ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR
FHWA	

**GENERAL NOTES**

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

DETAIL DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR MANHOLE COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ROUND FRAMES AND COVERS SHALL HAVE CONTINUOUSLY MACHINED BEARING SURFACES TO PREVENT ROCKING AND RATTLING.

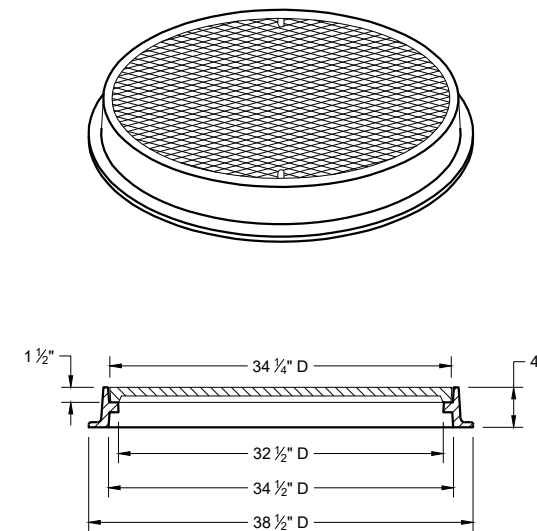
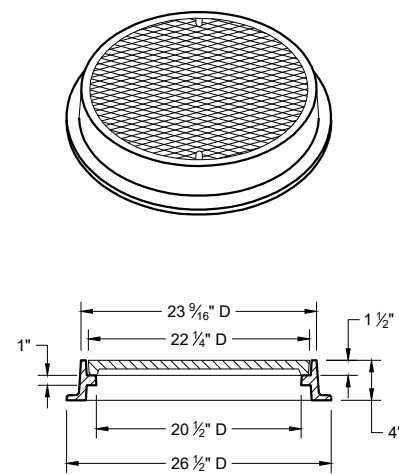
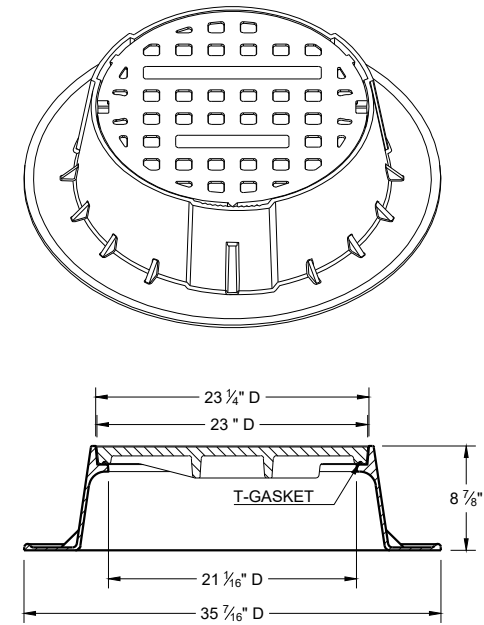
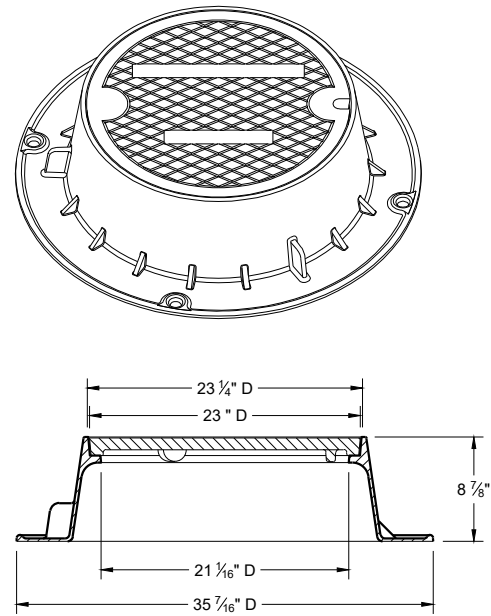


**TYPE "K"**

**INLET COVER TYPE "BW"**

6

6



**TYPE "J"**

NOTE: EITHER CASTING IS ACCEPTABLE

**TYPE "J" SPECIAL**

TYPE "B" NON-ROCKING SELF-SEAL LID (NOTED AS TYPE J-S ON THE DRAINAGE TABLE)

NOTE: EITHER CASTING IS ACCEPTABLE

**TYPE "L"**

**TYPE "M"**

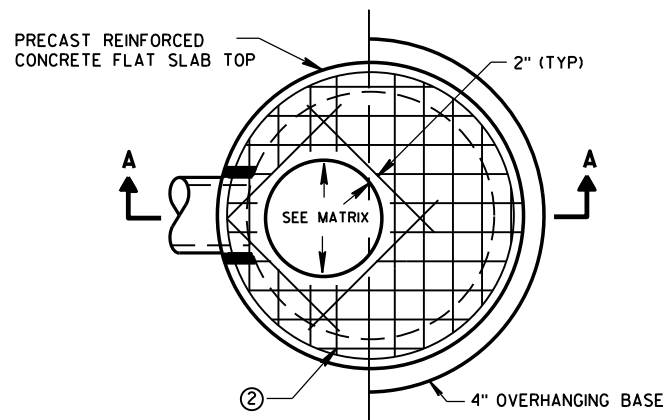
**INLET COVERS TYPES BW  
MANHOLE COVERS TYPES K,  
J, J-S, L, AND M**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

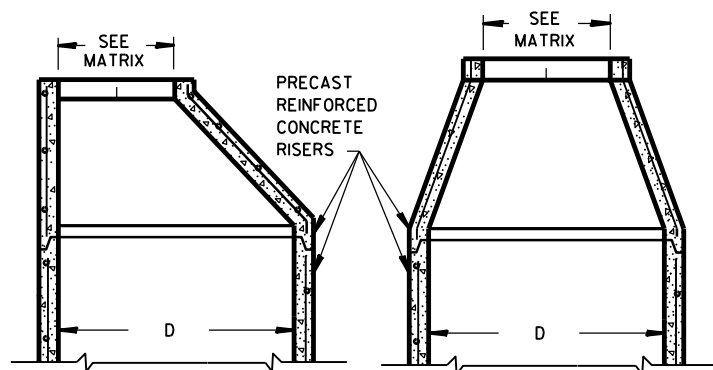
APPROVED  
July 2023 /S/ Rodney Taylor  
DATE ROADWAY STANDARDS DEVELOPMENT  
UNIT SUPERVISOR

SDD 08A05-20d

SDD 08A05-20d

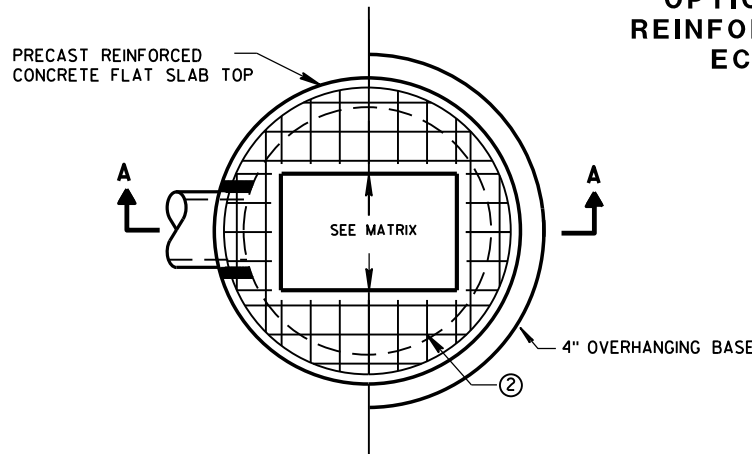


PLAN VIEW CIRCULAR OPENING

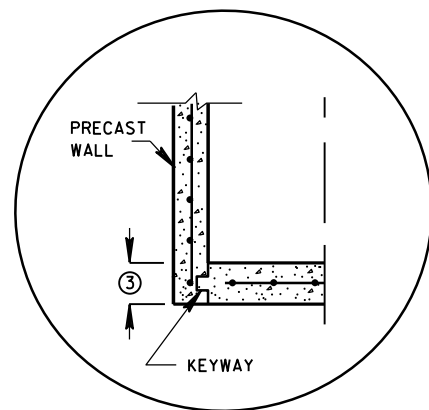


OPTIONAL PRECAST REINFORCED CONCRETE ECCENTRIC TOP

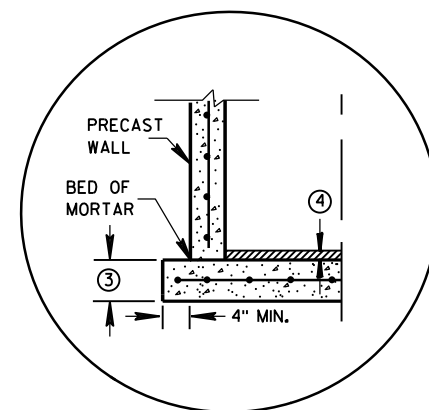
OPTIONAL PRECAST REINFORCED CONCRETE CONCENTRIC TOP



PLAN VIEW RECTANGULAR OPENING

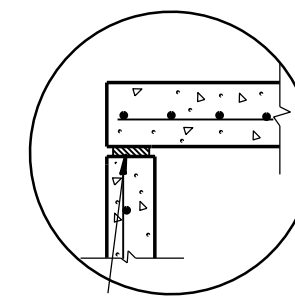


PRECAST REINFORCED CONCRETE WITH INTEGRAL BASE OPTION

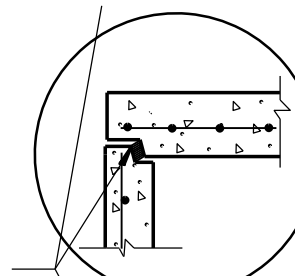


SEPARATE PRECAST REINFORCED CONCRETE BASE OPTION

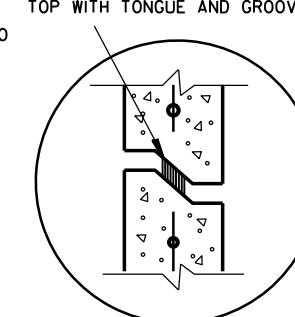
DETAIL "A"



TOP WITH PLAIN END JOINT

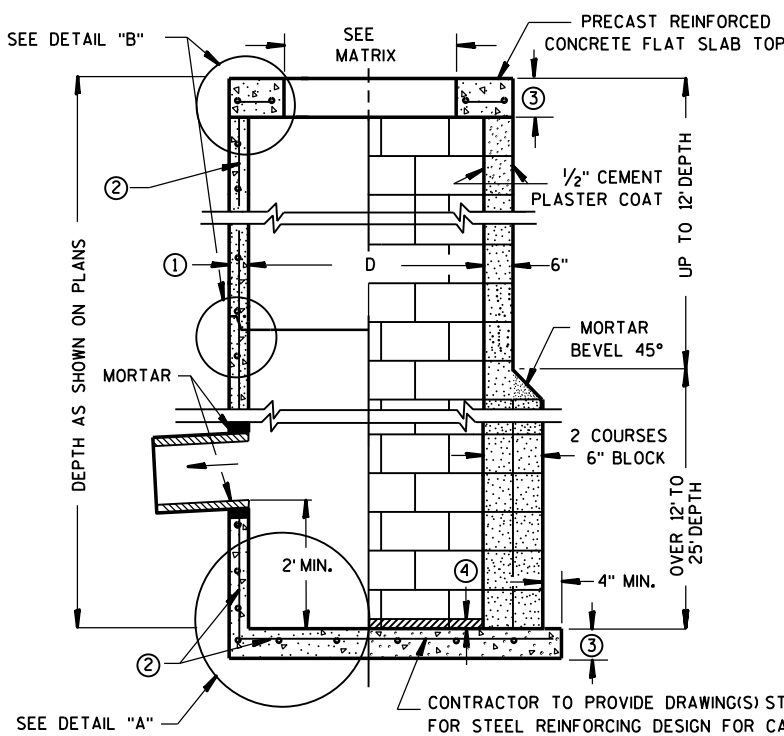


TOP WITH TONGUE AND GROOVE JOINT



RISER WITH TONGUE AND GROOVE JOINT

DETAIL "B"

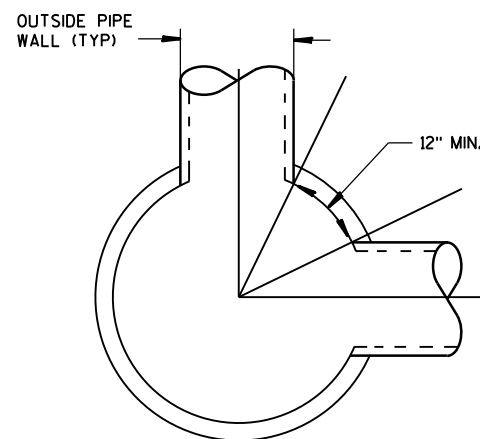


SECTION A-A

PRECAST REINFORCED CONCRETE WITH MONOLITHIC BASE

CONCRETE BLOCK WITH CAST-IN-PLACE OR PRECAST REINFORCED CONCRETE BASE ②

JOINTS TO BE SEALED WITH A BUTYL RUBBER SEAL PER SEALANT MANUFACTURERS RECOMMENDATIONS CONFORMING TO ASTM C 990 (TYP)



DETAIL "C"

**GENERAL NOTES**

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS. UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST CATCH BASIN UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL DRAINAGE STRUCTURES ARE DESIGNATED ON THE PLANS AS "MANHOLES 3X3-L", "CATCH BASINS 4-B", "INLETS 2X3-H", ETC. THE FIRST NUMBERS DESIGNATE THE SIZE OF THE STRUCTURE, AND THE FOLLOWING LETTER DESIGNATES THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF FOUNDATION BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

PRECAST REINFORCED CONCRETE CONE TOPS (ECCENTRIC OR CONCENTRIC) OR PRECAST REINFORCED CONCRETE FLAT SLAB TOPS MAY BE USED ON CONCRETE BLOCK STRUCTURES.

ECCENTRIC CONE TOPS MAY BE USED ON ALL STRUCTURES, AND CONCENTRIC CONE TOPS SHALL BE USED ONLY ON STRUCTURES 5 FEET OR LESS IN DEPTH, UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

STEPS MEETING AASHTO M199 AND THE FOLLOWING REQUIREMENTS SHALL BE INSTALLED IN ALL STRUCTURES OVER 5 FEET IN DEPTH; 16 INCH C-C MAXIMUM SPACING; PROJECT A MINIMUM CLEAR DISTANCE OF 4 INCHES FROM THE WALL AT THE POINT OF EMBEDMENT; MINIMUM LENGTH OF 10 INCHES; MINIMUM WALL EMBEDMENT OF 3 INCHES. FERROUS METAL STEPS NOT PAINTED OR TREATED TO RESIST CORROSION SHALL HAVE A MINIMUM CROSS SECTIONAL DIMENSION OF 1 INCH.

STEPS OF APPROVED POLYPROPYLENE PLASTIC COATED REINFORCEMENT BAR ARE ACCEPTABLE. REINFORCING BAR MUST BE A MINIMUM OF 1/2 INCH AND MEET THE REQUIREMENTS OF ASTM A615.

CERTIFICATION SHALL BE PROVIDED THAT INSTALLED STEPS WHEN TESTED IN ACCORDANCE WITH SECTION 10 OF AASHTO T280 CAN WITHSTAND A VERTICAL LOAD OF 800 LBS. AND A HORIZONTAL LOAD OF 400 LBS.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

ALL PRECAST INLET UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF AASHTO DESIGNATION M199.

PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

CONCRETE BLOCK WILL NOT BE PERMITTED FOR STRUCTURES GREATER THAN 4 FEET IN DIAMETER.

4" OVERHANGING BASES ARE REQUIRED FOR ALL CONCRETE BLOCK INSTALLATIONS. 4" OVERHANG IS REQUIRED WHEN SEPARATE PRECAST BASE IS PROVIDED. OVERHANG IS NOT REQUIRED ON PRECAST STRUCTURES WITH AN INTEGRAL OR MONOLITHIC BASE.

FOR ADDITIONAL CONFIGURATIONS, MAINTAIN A MINIMUM OF 12 INCHES AS MEASURED FROM THE INSIDE OF THE STRUCTURE WALL BETWEEN THE OUTSIDE PIPE WALLS OF ADJACENT PIPES. SEE DETAIL "C".

- ① MINIMUM WALL THICKNESS SHALL BE 4 INCHES FOR 3-FT, 5 INCHES FOR 4-FT, 6 INCHES FOR 5-FT AND 7 INCHES FOR 6-FT DIAMETER PRECAST CATCH BASINS.
- ② FOR PRECAST CATCH BASINS PROVIDE REINFORCING STEEL IN ACCORDANCE TO AASHTO M199.
- ③ PRECAST FLAT SLAB TOPS AND BASES WITH A DIAMETER OF 48" AND LESS SHALL HAVE A MINIMUM THICKNESS OF 6". PRECAST FLAT SLAB TOPS AND BASES WITH A DIAMETER LARGER THAN 48" SHALL HAVE A MINIMUM THICKNESS OF 8".
- ④ 1" CONCRETE KEY POURED AFTER INSTALLATION. 2" SUMP MEASURED FROM TOP OF KEY.

**CATCH BASIN COVER OPENING MATRIX**

CATCH BASIN SIZE	INLET COVER TYPE OPENING SIZE (FT)	ALL A'S	ALL B'S	BW	C	F	ALL H'S	S	T	V	WM	Z
3-FT	2X2	X	X					X		X		
	2 DIA.				X					X		X
4-FT- 6-FT	2X2	X	X					X		X		
	2X2.5			X				X	X	X	X	
	2 DIA.				X							X
	2X3						X					
	2.5X3											

**PIPE MATRIX**

CATCH BASIN SIZE	MAXIMUM INSIDE PIPE DIAMETER FOR TWO PIPES	
	180° SEPARATION (IN)	90° SEPARATION (IN)
3-FT	15	12
4-FT	24	18
5-FT	36	24
6-FT	42	30

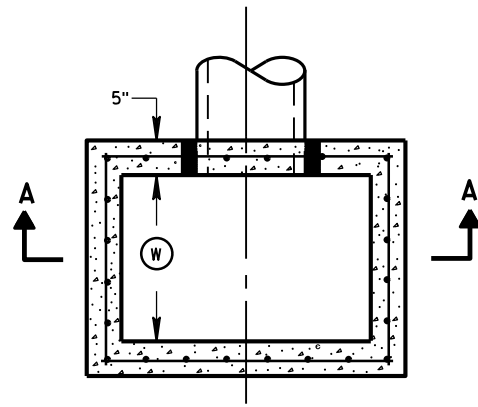
CATCH BASINS 3-FT, 4-FT, 5-FT AND 6-FT DIAMETER

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

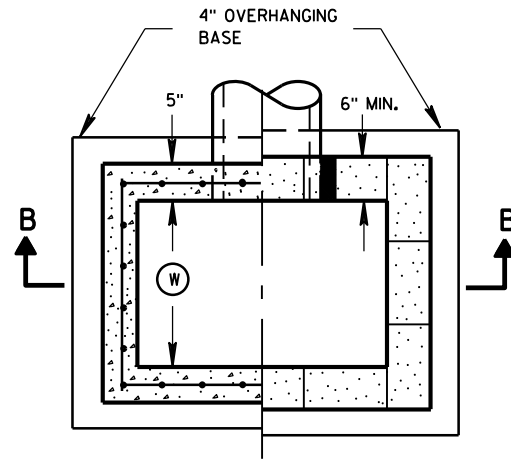
APPROVED  
Sep 1, 2016 /S/ Rodney Taylor  
DATE ROADWAY STANDARDS DEVELOPMENT  
FHWA UNIT SUPERVISOR

**CATCH BASINS 3-FT, 4-FT, 5-FT AND 6-FT DIAMETER**

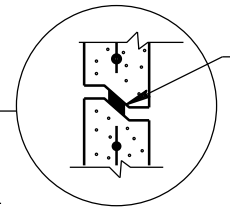




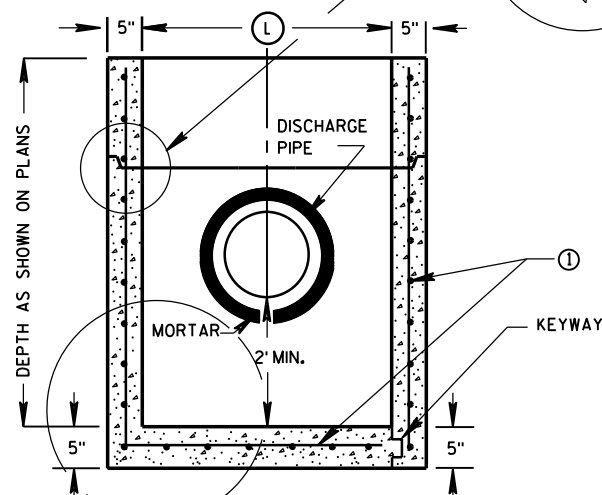
PLAN VIEW



PLAN VIEW

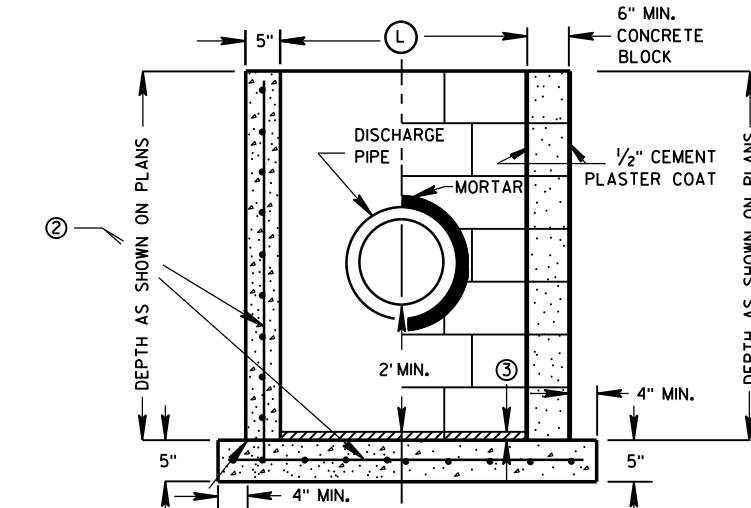


RISER JOINTS TO BE SEALED WITH A BUTYL RUBBER SEAL PER SEALANT MANUFACTURERS RECOMMENDATIONS CONFORMING TO ASTM C 990 (TYP)



PRECAST REINFORCED CONCRETE WITH MONOLITHIC BASE

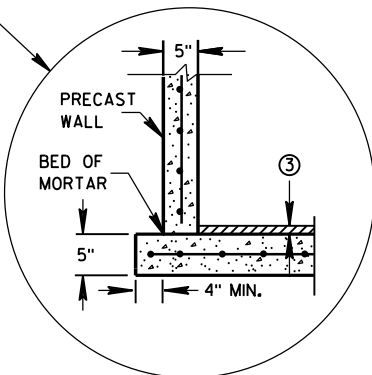
SECTION A-A



CAST-IN-PLACE REINFORCED CONCRETE

CONCRETE BLOCK ON CAST-IN-PLACE WITH PRECAST REINFORCED CONCRETE BASE ①

SECTION B-B



SEPARATE PRECAST REINFORCED CONCRETE BASE OPTION

**GENERAL NOTES**

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST CATCH BASIN UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL PRECAST CATCH BASIN UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF ASTM C 913.

ALL DRAINAGE STRUCTURES ARE DESIGNATED ON THE PLANS AS "MANHOLES 3X3-L", "CATCH BASINS 4-B", "INLETS 2X3-H", ETC. THE FIRST NUMBERS DESIGNATES THE SIZE OF THE STRUCTURE, AND THE FOLLOWING LETTER DESIGNATES THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF FOUNDATION BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

4" OVERHANGING BASES ARE REQUIRED FOR CAST-IN-PLACE REINFORCED CONCRETE AND CONCRETE BLOCK INSTALLATIONS. 4" OVERHANG IS REQUIRED WHEN SEPARATE PRECAST BASE IS PROVIDED. OVERHANG IS NOT REQUIRED ON PRECAST STRUCTURES WITH AN INTEGRAL OR MONOLITHIC BASE.

MAXIMUM INSIDE PIPE DIAMETER DETERMINED BY 3" CLEARANCE ON EACH SIDE OF THE OUTSIDE WALL OF THE PIPE. SEE DETAIL "A". ASSUMES PIPE ENTERS PERPENDICULAR TO THE STRUCTURE.

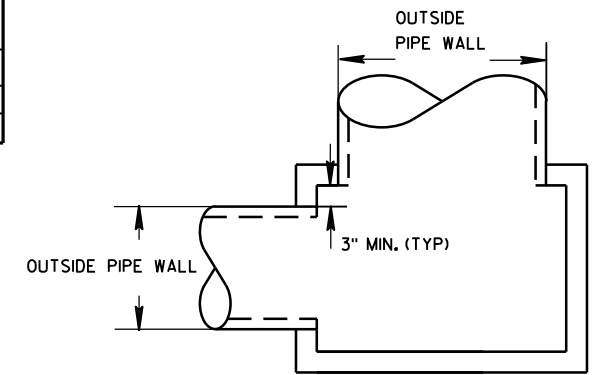
- ① FOR PRECAST CATCH BASINS PROVIDE REINFORCING STEEL IN ACCORDANCE TO ASTM C 913.
- ② CONTRACTOR TO PROVIDE DRAWING(S) STAMPED BY A PROFESSIONAL ENGINEER FOR STEEL REINFORCING DESIGN FOR CAST-IN-PLACE STRUCTURES.
- ③ 1" CONCRETE KEY POURED AFTER INSTALLATION. 2" SUMP MEASURED FROM TOP OF KEY.

**CATCH BASIN COVER MATRIX**

CATCH BASIN SIZE	INLET COVER TYPE		F	ALL H'S
	WIDTH (W) (FT)	LENGTH (L) (FT)		
2X3-FT	2	3		X
2.5X3-FT	2.5	3	X	

**PIPE MATRIX**

CATCH BASIN SIZE	MAXIMUM INSIDE PIPE DIAMETER FOR TWO PIPES	
	WIDTH (IN)	LENGTH (IN)
2X3-FT	12	24
2.5X3-FT	18	24



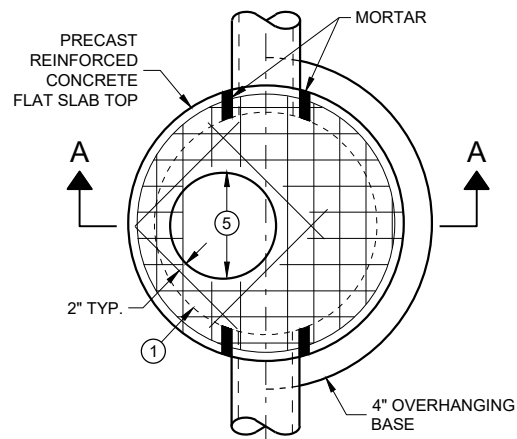
DETAIL "A"

CATCH BASINS 2X3-FT AND 2.5X3-FT

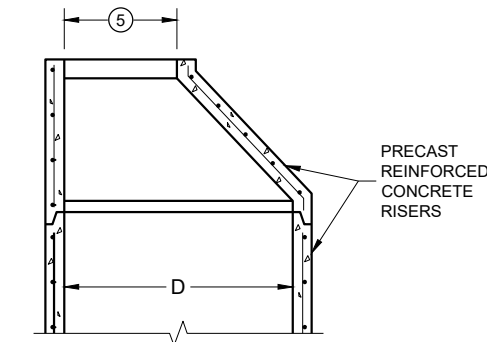
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
DATE: Sept., 2016 /S/ Rodney Taylor  
ROADWAY STANDARDS DEVELOPMENT  
UNIT SUPERVISOR

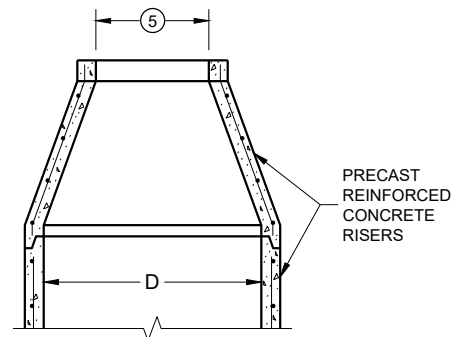
**CATCH BASINS 2X3-FT AND 2.5X3-FT**



**PLAN VIEW  
CIRCULAR OPENING**



**OPTIONAL PRECAST  
REINFORCED CONCRETE  
ECCENTRIC TOP**



**OPTIONAL PRECAST  
REINFORCED CONCRETE  
CONCENTRIC TOP**

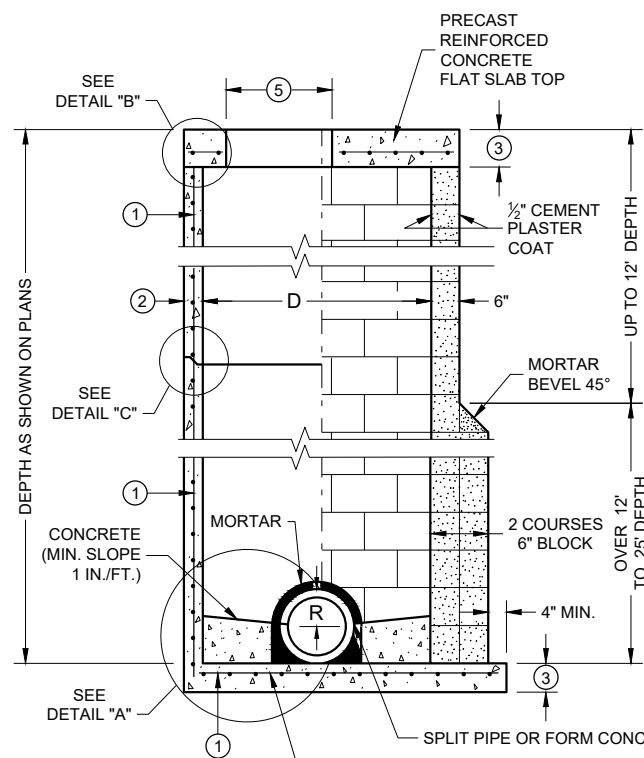
**MANHOLE COVER OPENING MATRIX**

MANHOLE COVER TYPE OPENING SIZE (FT.)	C	ALL J'S	K	L	M
2 DIA.	X	X		X	
3 DIA.			X		X

**PIPE MATRIX**

MANHOLE SIZE (DIA.)	MAXIMUM INSIDE PIPE DIAMETER FOR TWO PIPES		MINIMUM WALL THICKNESS (IN)	MINIMUM PRECAST FLAT SLAB TOP AND BASE THICKNESS
	180° SEPARATION (IN)	90° SEPARATION (IN)		
3-FT	15	12	4	6
4-FT	24	18	4	6
5-FT	36	24	5	8
6-FT	42	36	6	8
7-FT	48	36/42*	7	8
8-FT	60	42	8	8
9-FT	66	54	9	10
10-FT	72	60	10	10

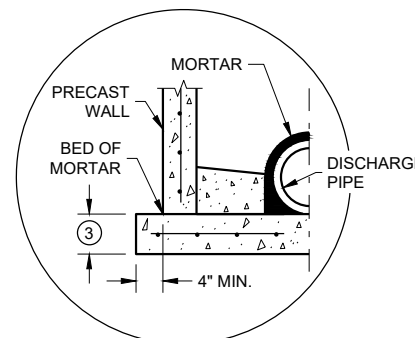
\*A 36" PIPE AND A 42" PIPE CAN BE PLACED WITHIN 90 DEGREES. SEE MINIMUM HORIZONTAL PIPE SEPARATION DETAIL.



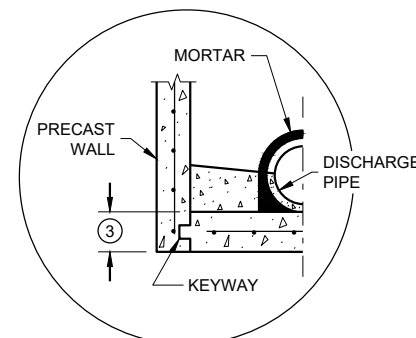
**SECTION A - A**

**PRECAST REINFORCED CONCRETE WITH MONOLITHIC BASE**

**CONCRETE BLOCK WITH CAST IN PLACE OR PRECAST REINFORCED CONCRETE BASE ①**

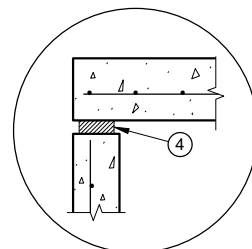


**SEPARATE PRECAST REINFORCED CONCRETE BASE OPTION**

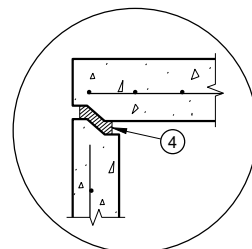


**PRECAST REINFORCED CONCRETE WITH INTEGRAL BASE OPTION**

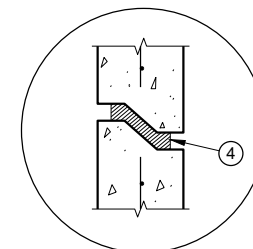
**DETAIL "A"**



**TOP WITH PLAIN END JOINT**



**TOP WITH TONGUE AND GROOVE JOINT**



**RISER WITH TONGUE AND GROOVE JOINT**

**DETAIL "B"**

**DETAIL "C"**

**GENERAL NOTES**

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST MANHOLE UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL DRAINAGE STRUCTURES ARE DESIGNATED ON THE PLANS AS "MANHOLES 3X3-L", "CATCH BASINS 4-B", "INLETS 2X3-H", ETC. THE FIRST NUMBERS DESIGNATE THE SIZE OF THE STRUCTURE, AND THE FOLLOWING LETTER DESIGNATES THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF FOUNDATION BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

PRECAST REINFORCED CONCRETE CONE TOPS (ECCENTRIC OR CONCENTRIC) OR PRECAST REINFORCED CONCRETE FLAT SLAB TOPS MAY BE USED ON CONCRETE BLOCK STRUCTURES.

ECCENTRIC CONE TOPS MAY BE USED ON ALL STRUCTURES. CONCENTRIC CONE TOPS SHALL BE USED ONLY ON STRUCTURES 5 FEET OR LESS IN DEPTH UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

STEPS MEETING AASHTO M199 AND THE FOLLOWING REQUIREMENTS SHALL BE INSTALLED IN ALL STRUCTURES OVER 5 FEET IN DEPTH: 16 INCH C-C MAXIMUM SPACING; PROJECT A MINIMUM CLEAR DISTANCE OF 4 INCHES FROM THE WALL AT THE POINT OF EMBEDMENT; MINIMUM LENGTH OF 10 INCHES; MINIMUM WALL EMBEDMENT OF 3 INCHES. FERROUS METAL STEPS NOT PAINTED OR TREATED TO RESIST CORROSION SHALL HAVE A MINIMUM CROSS SECTIONAL DIMENSION OF 1 INCH.

STEPS OF APPROVED POLYPROPYLENE PLASTIC COATED REINFORCEMENT BAR ARE ACCEPTABLE. REINFORCING BAR MUST BE A MINIMUM OF 1/2 INCH AND MEET THE REQUIREMENTS OF ASTM A615.

CERTIFICATION SHALL BE PROVIDED THAT INSTALLED STEPS WHEN TESTED IN ACCORDANCE WITH SECTION 10 OF AASHTO T280 CAN WITHSTAND A VERTICAL LOAD OF 800 LBS. AND A HORIZONTAL LOAD OF 400 LBS.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

ALL PRECAST MANHOLE UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF AASHTO DESIGNATION M199.

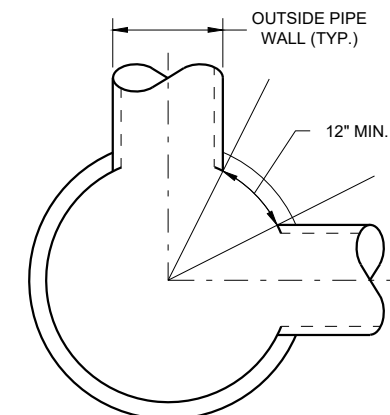
PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

CONCRETE BLOCK WILL NOT BE PERMITTED FOR STRUCTURES GREATER THAN 4 FEET IN DIAMETER.

4" OVERHANGING BASES ARE REQUIRED FOR ALL CONCRETE BLOCK INSTALLATIONS. 4" OVERHANG IS REQUIRED WHEN SEPARATE PRECAST BASE IS PROVIDED. OVERHANG IS NOT REQUIRED ON PRECAST STRUCTURES WITH AN INTEGRAL OR MONOLITHIC BASE.

FOR ADDITIONAL CONFIGURATIONS, MAINTAIN A MINIMUM OF 12 INCHES AS MEASURED FROM THE INSIDE OF THE STRUCTURE WALL BETWEEN THE OUTSIDE PIPE WALLS OF ADJACENT PIPES. SEE DETAIL "D".

- ① FOR PRECAST MANHOLES PROVIDE REINFORCING STEEL IN ACCORDANCE TO AASHTO M199.
- ② SEE PIPE MATRIX TABLE FOR MINIMUM WALL THICKNESS FOR PRECAST MANHOLES
- ③ SEE PIPE MATRIX TABLE FOR MINIMUM THICKNESS OF PRECAST FLAT SLAB TOPS AND BASES.
- ④ JOINTS TO BE SEALED WITH A BUTYL RUBBER SEAL PER SEALANT MANUFACTURERS RECOMMENDATIONS CONFORMING TO ASTM C 990 (TYP.).
- ⑤ SEE MANHOLE COVER OPENING MATRIX.

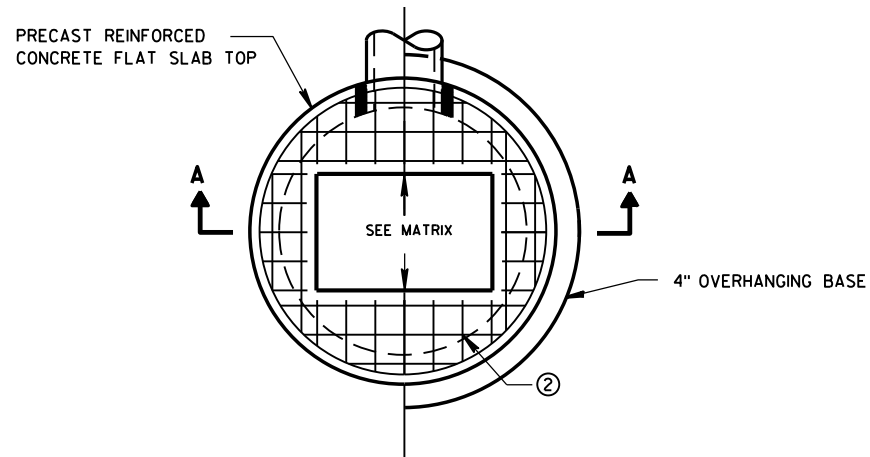


**MINIMUM HORIZONTAL PIPE SEPARATION**

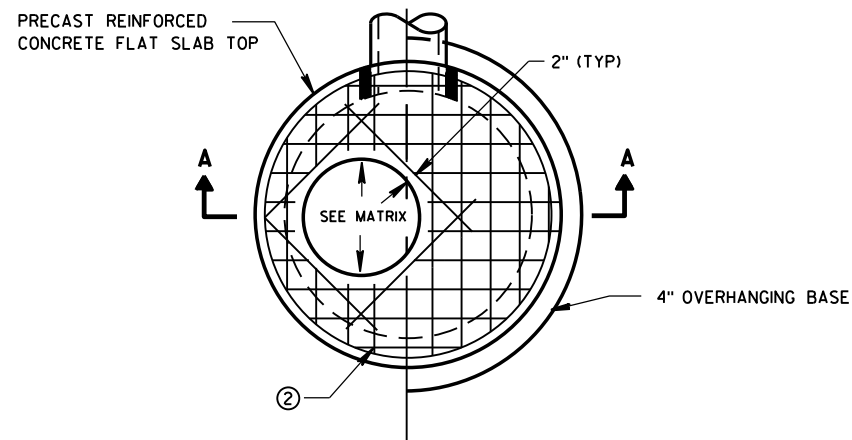
**MANHOLES, 3-FT, 4-FT  
5-FT, 6-FT, 7-FT, 8-FT, 9-FT  
AND 10-FT DIAMETER**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
November 2021 /S/ Rodney Taylor  
DATE ROADWAY STANDARDS DEVELOPMENT ENGINEER  
FHWA



PLAN VIEW RECTANGULAR OPENING



PLAN VIEW CIRCULAR OPENING

**GENERAL NOTES**

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST INLET UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL DRAINAGE STRUCTURES ARE DESIGNATED ON THE PLANS AS "MANHOLES 3X3-L", "CATCH BASINS 4-B", "INLETS 2X3-H", ETC. THE FIRST NUMBERS DESIGNATE THE SIZE OF THE STRUCTURE, AND THE FOLLOWING LETTER DESIGNATES THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF FOUNDATION BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

ALL PRECAST INLET UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF AASHTO DESIGNATION M199.

PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

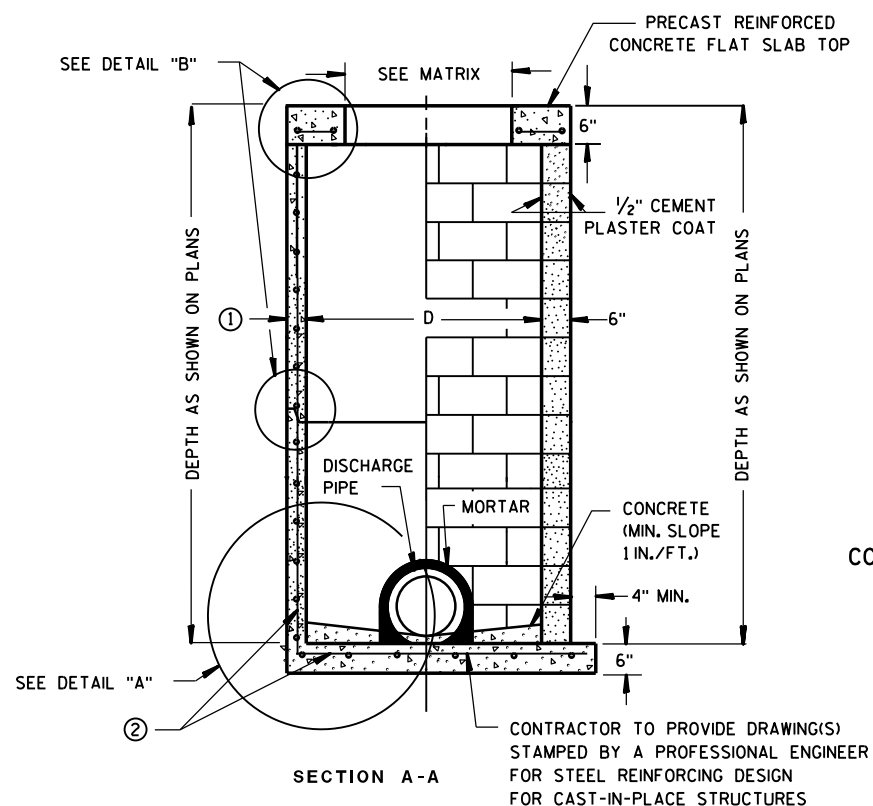
4" OVERHANGING BASES ARE REQUIRED FOR ALL CONCRETE BLOCK INSTALLATIONS. 4" OVERHANG IS REQUIRED WHEN SEPARATE PRECAST BASE IS PROVIDED. OVERHANG IS NOT REQUIRED ON PRECAST STRUCTURES WITH AN INTEGRAL OR MONOLITHIC BASE.

FOR ADDITIONAL CONFIGURATIONS, MAINTAIN A MINIMUM OF 12 INCHES AS MEASURED FROM THE INSIDE OF THE STRUCTURE WALL BETWEEN THE OUTSIDE PIPE WALLS OF ADJACENT PIPES. SEE DETAIL "C".

- ① MINIMUM WALL THICKNESS SHALL BE 4-IN FOR 3-FT DIAMETER AND 5-IN FOR 4-FT DIAMETER PRECAST INLETS.
- ② FOR PRECAST CATCH BASINS PROVIDE REINFORCING STEEL IN ACCORDANCE TO AASHTO M199.

**INLET COVER OPENING MATRIX**

	INLET COVER TYPE	ALL A'S	ALL B'S	BW	C	F	ALL H'S	S	T	V	WM	Z
3-FT	2 DIA.				X							X
	2X2	X	X					X		X		
4-FT	2 DIA.				X							X
	2X2	X	X					X		X	X	
	2X2.5			X								
	2X3						X					
	2.5X3					X						

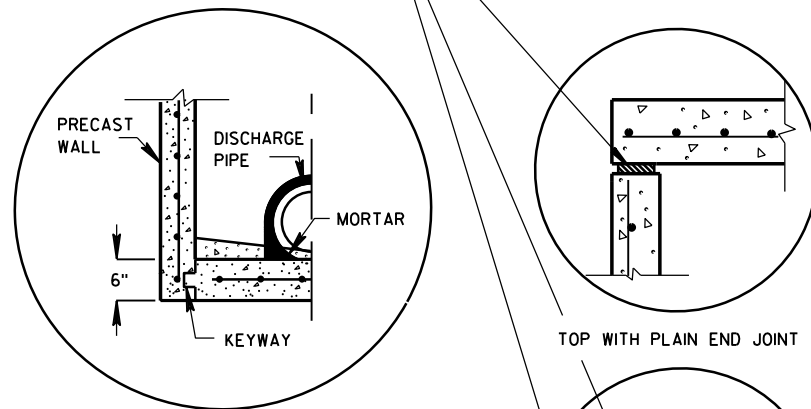


**PRECAST REINFORCED CONCRETE WITH MONOLITHIC BASE**      **CONCRETE BLOCK WITH CAST-IN-PLACE OR PRECAST REINFORCED CONCRETE BASE ②**

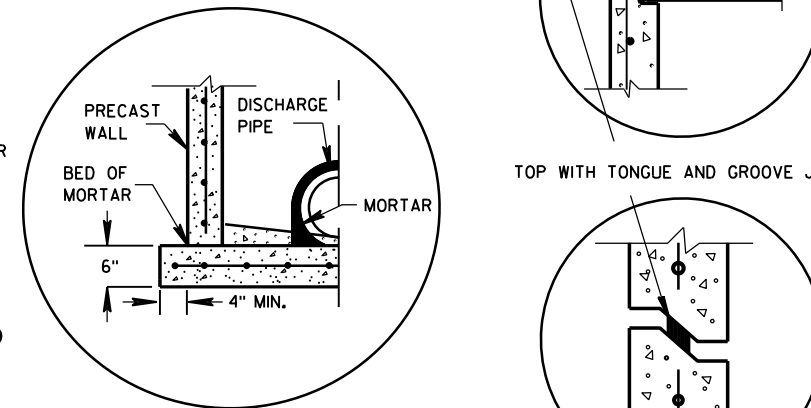
CIRCULAR INLETS W/ FLAT TOP

CONTRACTOR TO PROVIDE DRAWING(S) STAMPED BY A PROFESSIONAL ENGINEER FOR STEEL REINFORCING DESIGN FOR CAST-IN-PLACE STRUCTURES

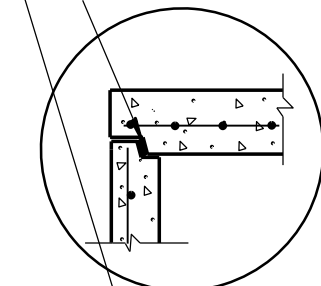
JOINTS TO BE SEALED WITH A BUTYL RUBBER SEAL PER SEALANT MANUFACTURERS RECOMMENDATIONS CONFORMING TO ASTM C990 (TYP)



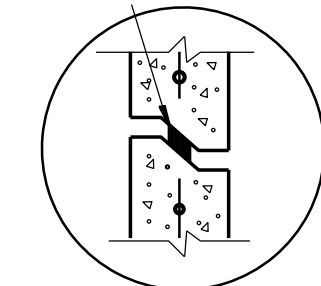
PRECAST REINFORCED CONCRETE WITH INTEGRAL BASE OPTION



SEPARATE PRECAST REINFORCED CONCRETE BASE OPTION



RISER WITH TONGUE AND GROOVE JOINT

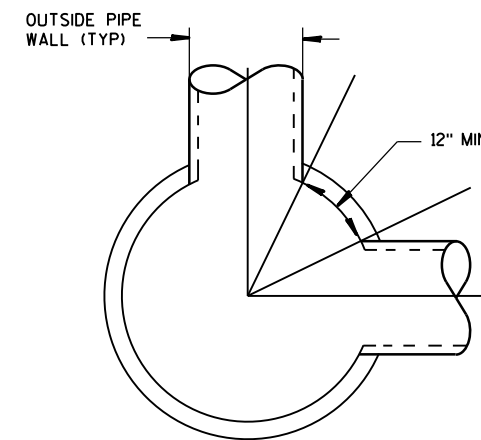


RISER WITH TONGUE AND GROOVE JOINT

DETAIL "A"

DETAIL "B"

INLETS 3-FT AND 4-FT DIAMETER



DETAIL "C"

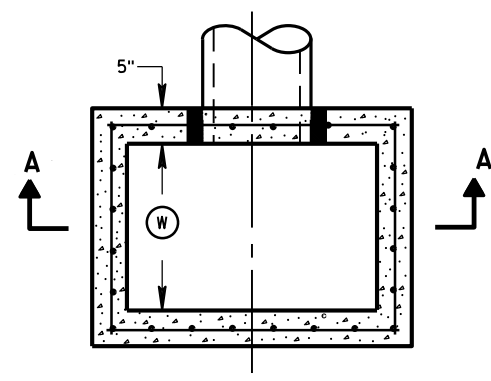
**PIPE MATRIX**

INLET SIZE	MAXIMUM INSIDE PIPE DIAMETER FOR TWO PIPES	
	180° SEPARATION (IN)	90° SEPARATION (IN)
3-FT	15	12
4-FT	24	18

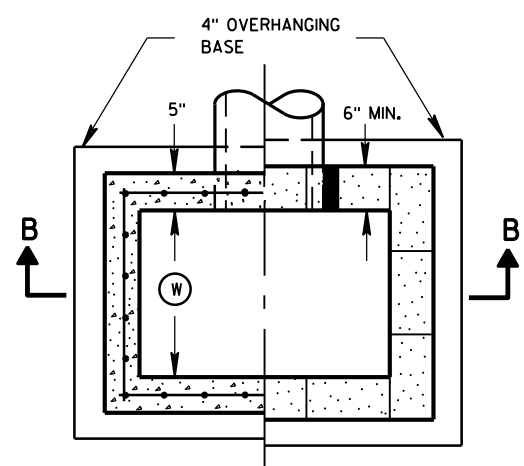
INLETS 3-FT AND 4-FT DIAMETER

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

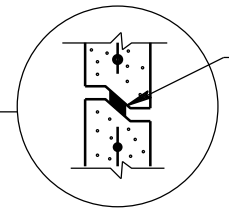
APPROVED  
 Sept., 2016 /S/ Rodney Taylor  
 DATE ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR  
 FHWA



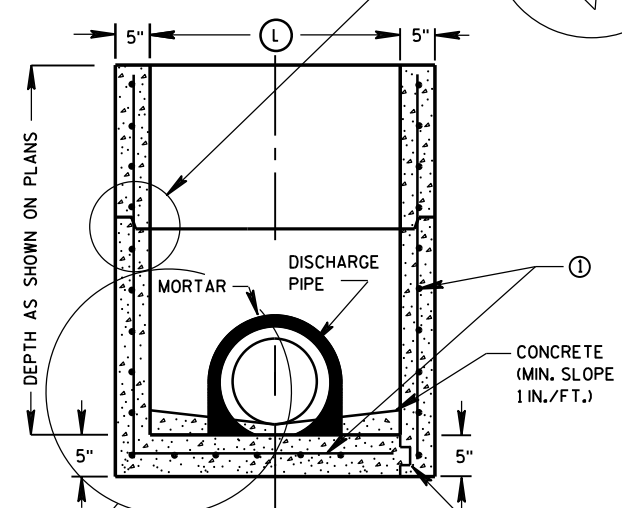
PLAN VIEW



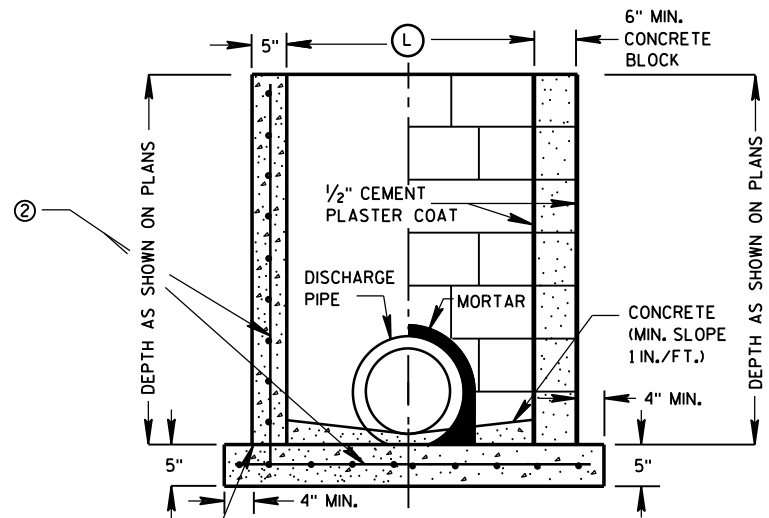
PLAN VIEW



RISER JOINTS TO BE SEALED WITH A BUTYL RUBBER SEAL PER SEALANT MANUFACTURERS RECOMMENDATIONS CONFORMING TO ASTM C 990 (TYP)



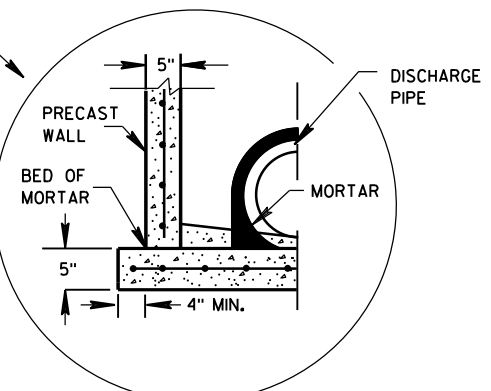
SECTION A-A



SECTION B-B

PRECAST REINFORCED CONCRETE WITH MONOLITHIC BASE  
 PRECAST REINFORCED CONCRETE WITH INTEGRAL BASE  
 KEYWAY

CAST-IN-PLACE REINFORCED CONCRETE  
 CONCRETE BLOCK WITH CAST-IN-PLACE OR PRECAST REINFORCED CONCRETE BASE ①



SEPARATE PRECAST REINFORCED CONCRETE BASE OPTION

**GENERAL NOTES**

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

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DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL PRECAST INLET UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF ASTM C 913.

ALL DRAINAGE STRUCTURES ARE DESIGNATED ON THE PLANS AS "MANHOLES 3X3-L", "CATCH BASINS 4-B", "INLETS 2X3-H", ETC. THE FIRST NUMBERS DESIGNATES THE SIZE OF THE STRUCTURE, AND THE FOLLOWING LETTER DESIGNATES THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF FOUNDATION BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

4" OVERHANGING BASES ARE REQUIRED FOR CAST-IN-PLACE REINFORCED CONCRETE AND CONCRETE BLOCK INSTALLATIONS. 4" OVERHANG IS REQUIRED WHEN SEPARATE PRECAST BASE IS PROVIDED. OVERHANG IS NOT REQUIRED ON PRECAST STRUCTURES WITH AN INTEGRAL OR MONOLITHIC BASE.

MAXIMUM INSIDE PIPE DIAMETER DETERMINED BY 3 INCH CLEARANCE ON EACH SIDE OF THE OUTSIDE WALL OF THE PIPE. SEE DETAIL "A". ASSUMES PIPE ENTERS PERPENDICULAR TO THE STRUCTURE.

① FOR PRECAST INLETS PROVIDE REINFORCING STEEL IN ACCORDANCE TO ASTM C 913.

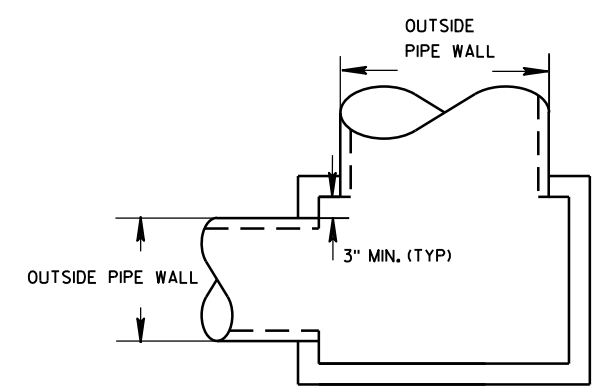
② CONTRACTOR TO PROVIDE DRAWING(S) STAMPED BY A PROFESSIONAL ENGINEER FOR STEEL REINFORCING DESIGN FOR CAST-IN-PLACE STRUCTURES.

**INLET COVER MATRIX**

INLET SIZE	INLET COVER TYPE		ALL A'S	ALL B'S	BW	F	ALL H'S	S	T	V	WM
	WIDTH ① (FT)	LENGTH ② (FT)									
2X2-FT	2	2	X	X				X		X	
2X2.5-FT	2	2.5			X			X	X	X	X
2X3-FT	2	3					X				
2.5X3-FT	2.5	3				X					

**PIPE MATRIX**

INLET SIZE	MAXIMUM INSIDE PIPE DIAMETER	
	WIDTH (IN)	LENGTH (IN)
2X2-FT	12	12
2X2.5-FT	12	18
2X3-FT	12	24
2.5X3-FT	18	24



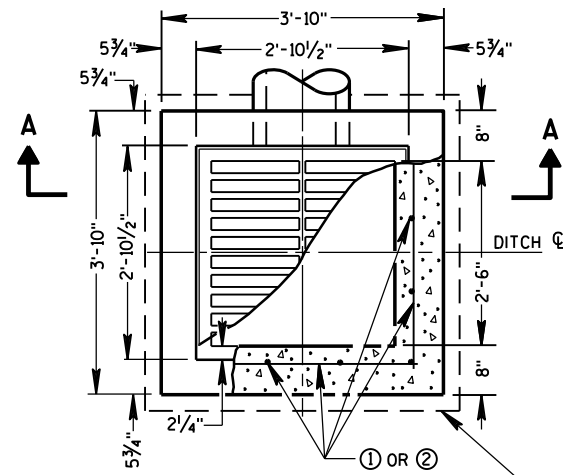
DETAIL "A"

**INLETS 2X2-FT, 2X2.5-FT, 2X3-FT AND 2.5X3-FT**

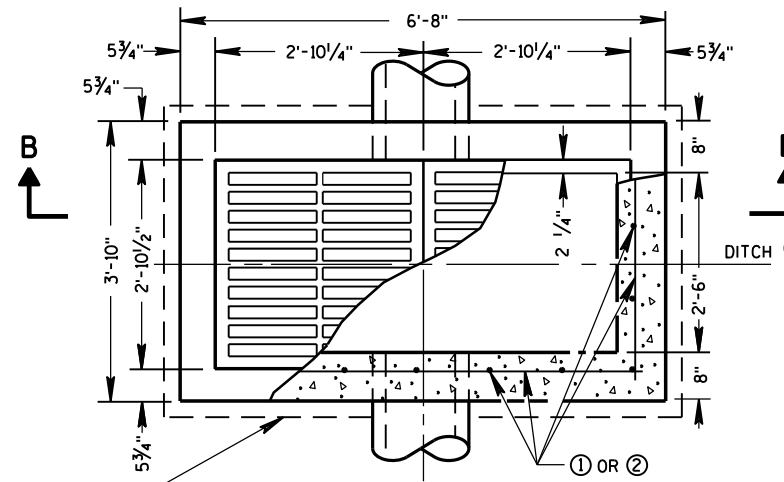
INLETS 2X2-FT, 2X2.5-FT, 2X3-FT AND 2.5X3-FT

STATE OF WISCONSIN  
 DEPARTMENT OF TRANSPORTATION

APPROVED  
 Sept., 2016 /S/ Rodney Taylor  
 DATE ROADWAY STANDARDS DEVELOPMENT  
 FHWA UNIT SUPERVISOR

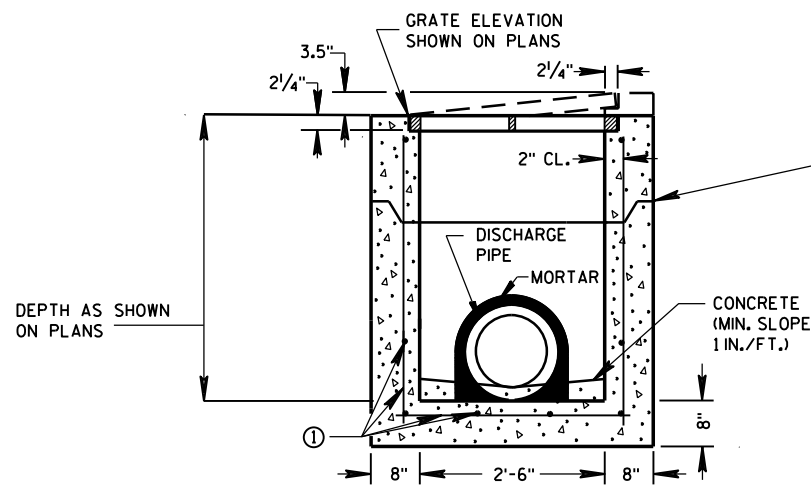


PLAN VIEW

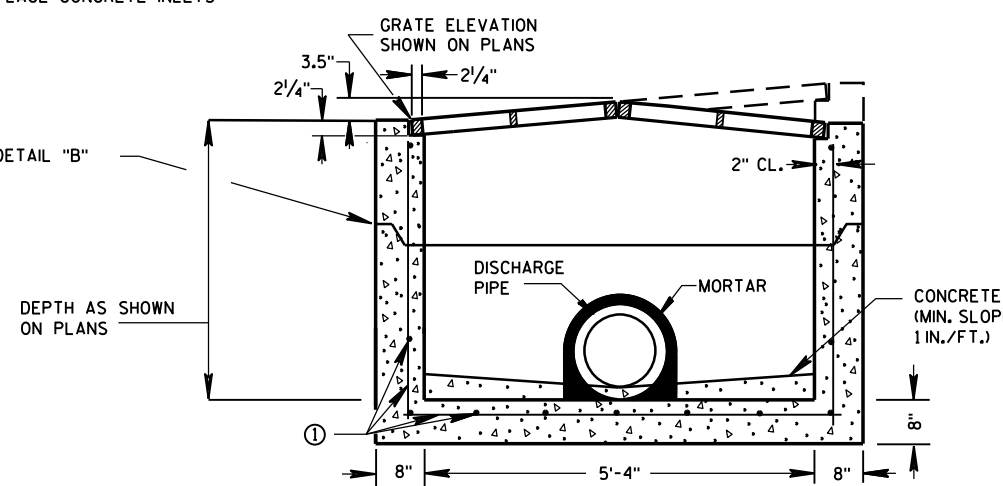


PLAN VIEW

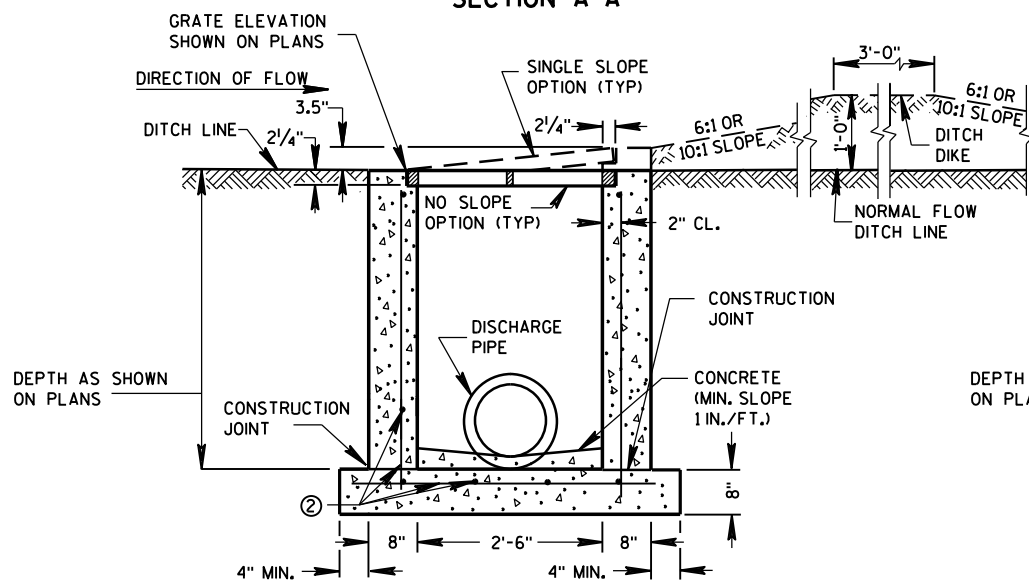
4" OVERHANGING BASE ON REINFORCED CAST-IN-PLACE CONCRETE INLETS



PRECAST REINFORCED CONCRETE SECTION A-A

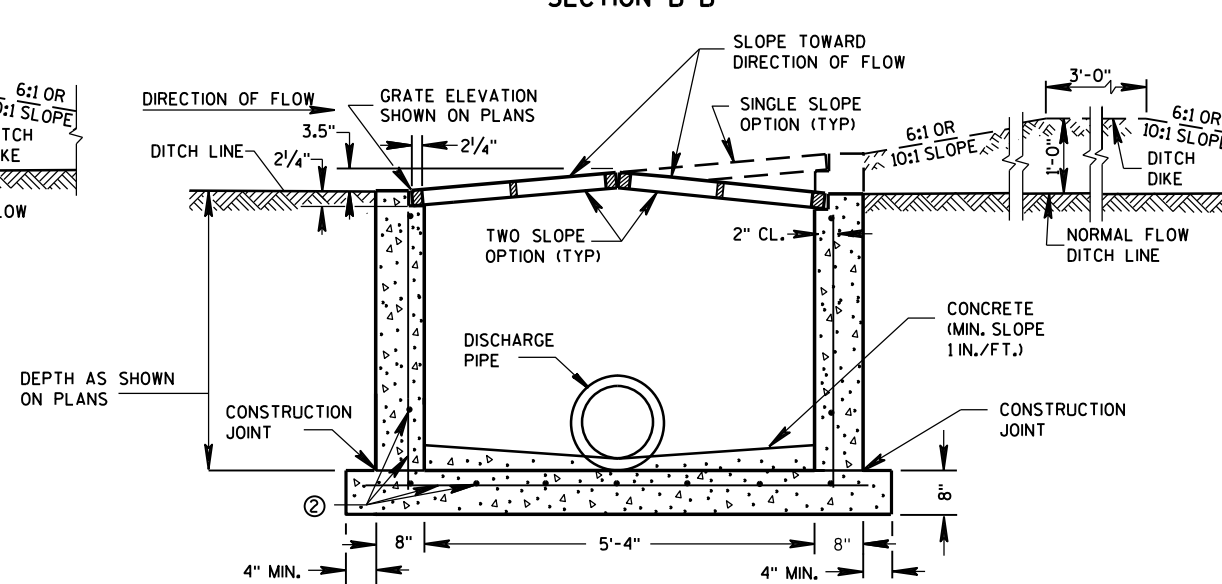


PRECAST REINFORCED CONCRETE SECTION B-B



REINFORCED CAST-IN-PLACE CONCRETE SECTION A-A

INLETS MEDIAN 1 GRATE



REINFORCED CAST-IN-PLACE CONCRETE SECTION B-B

INLETS MEDIAN 2 GRATE

**GENERAL NOTES**

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UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST INLET UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR INLETS WHICH MAY INCLUDE PRECAST REINFORCED CONCRETE INLETS, SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL MEDIAN INLETS ARE DESIGNATED ON THE PLANS AS "INLETS, IG-MS", ETC. THE FIRST NUMBER AND LETTER DESIGNATE THE TYPE OF STRUCTURE, AND THE FOLLOWING LETTERS DESIGNATE THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT. BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF FOUNDATION BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

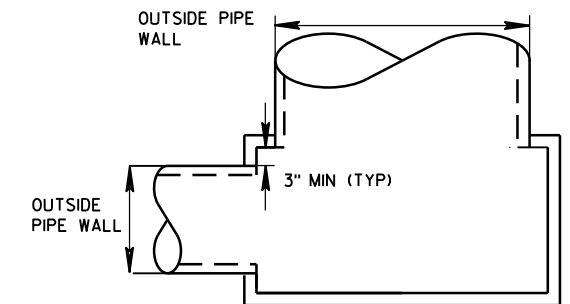
ALL PRECAST INLET UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF ASTM C 913.

MAXIMUM INSIDE PIPE DIAMETER DETERMINED BY 3" CLEARANCE ON EACH SIDE OF THE OUTSIDE WALL OF THE PIPE. SEE DETAIL "A". ASSUMES PIPE ENTERS PERPENDICULAR TO THE STRUCTURE.

- ① FOR PRECAST INLETS PROVIDE REINFORCING STEEL IN ACCORDANCE TO ASTM C 913.
- ② CONTRACTOR TO PROVIDE DRAWING(S) STAMPED BY A PROFESSIONAL ENGINEER FOR STEEL REINFORCING DESIGN FOR CAST-IN-PLACE STRUCTURES.

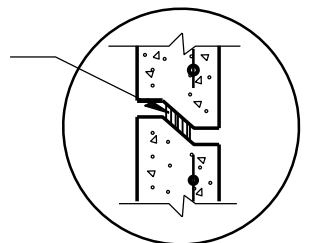
**PIPE MATRIX**

INLET SIZE	MAXIMUM INSIDE PIPE DIAMETER	
	WIDTH (IN)	LENGTH (IN)
1 GRATE	18	18
2 GRATE	18	42



DETAIL "A"

JOINTS TO BE SEALED WITH A BUTYL RUBBER SEAL PER SEALANT MANUFACTURERS RECOMMENDATIONS CONFORMING TO ASTM C 990 (TYP)

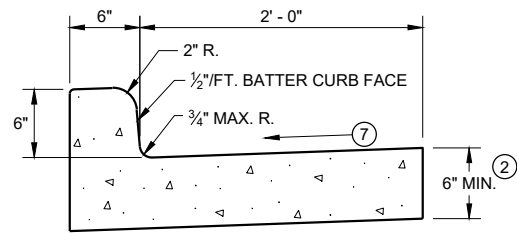


DETAIL "B"

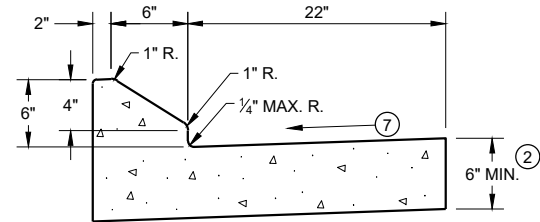
**INLETS MEDIAN 1 AND 2 GRATE**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

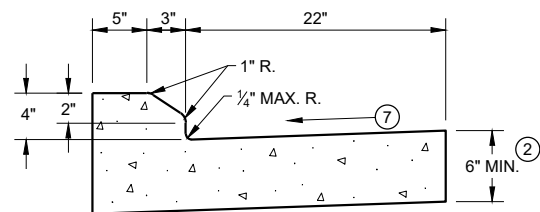
APPROVED  
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FHWA UNIT SUPERVISOR



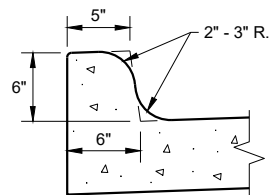
**TYPES A<sup>1</sup> & D**



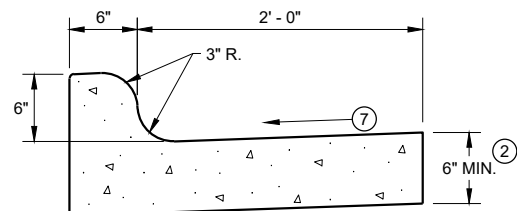
**6" SLOPED CURB TYPES G<sup>1</sup> & J**



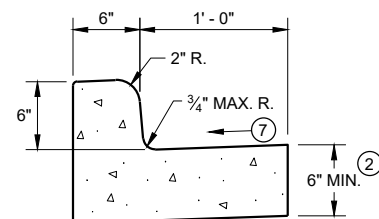
**4" SLOPED CURB TYPES G<sup>1</sup> & J**



**TYPES K<sup>1</sup> & L**  
(OPTIONAL CURB SHAPE)

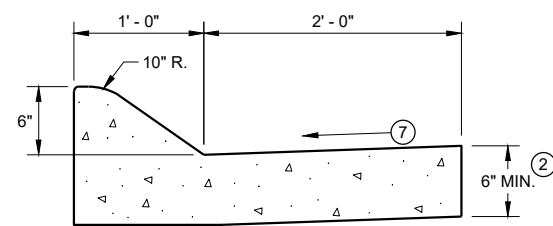


**TYPES K<sup>1</sup> & L**  
**CONCRETE CURB AND GUTTER 30"**

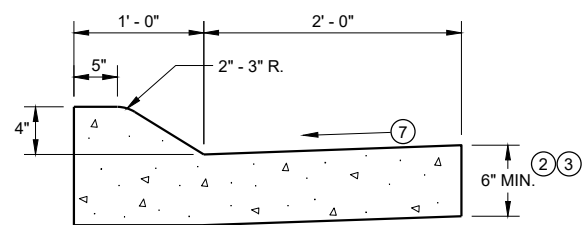


**TYPES A<sup>1</sup> & D**

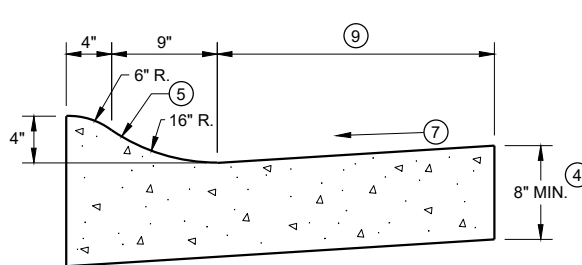
**CONCRETE CURB AND GUTTER 18"**



**6" SLOPED CURB TYPES A<sup>1</sup> & D**

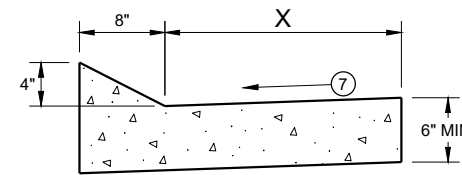


**4" SLOPED CURB TYPES A<sup>1</sup> & D**  
**CONCRETE CURB AND GUTTER 36"**



**4" SLOPED CURB TYPES R<sup>1</sup> & T**

TBT & TBTT	X
30"	22"
36"	28"

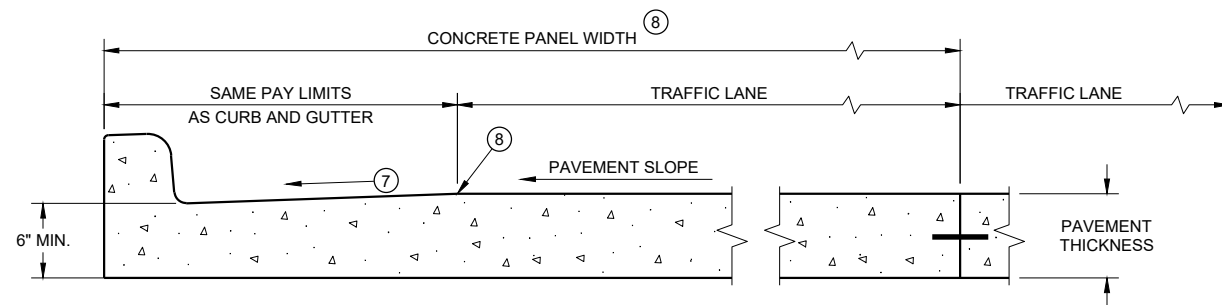


**TYPES TBT & TBTT<sup>1</sup>**

**CONCRETE CURB AND GUTTER**

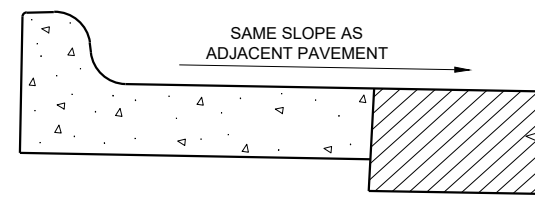
**PAVEMENT THICKNESS AND MAXIMUM CONCRETE PANEL WIDTH TABLE**

PAVEMENT THICKNESS	MAXIMUM PANEL WIDTH
LESS THAN 10"	12'
10" & ABOVE	15'



**PARTIAL SECTION OF PAVEMENT\* WITH INTEGRAL CURB AND GUTTER**

\* BIKE LANE IS NOT SHOWN



**REVERSE SLOPE GUTTER<sup>6</sup>**  
(TYPICAL FOR ALL CURB & GUTTER TYPES)

**GENERAL NOTES**

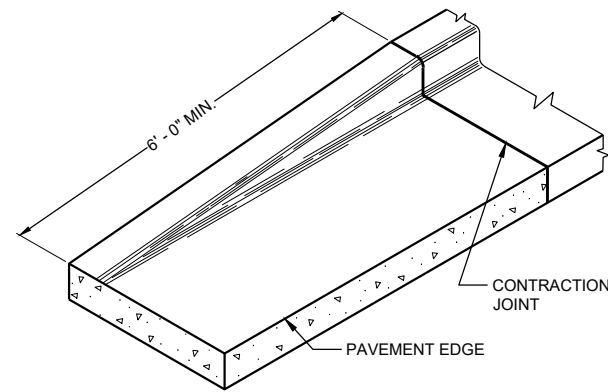
DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

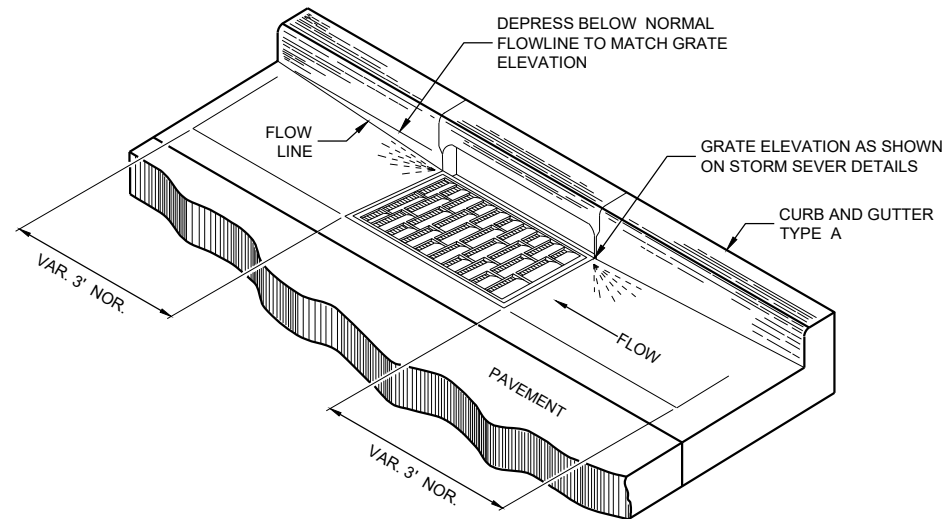
INTEGRAL CURB AND GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB AND GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ③ USE 8" MINIMUM GUTTER THICKNESS WHEN USED WITH AN ADJACENT CONCRETE TRUCK APRON PLACED BEHIND BACK OF CURB.
- ④ THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑤ UNLESS OTHERWISE NOTED, FOR STAKING PURPOSES THE FACE OF CURB IS 6" FROM THE BACK OF CURB.
- ⑥ WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.
- ⑦ USE 4% GUTTER CROSS SLOPE UNLESS OTHERWISE NOTED IN THE PLANS.
- ⑧ INCLUDE LONGITUDINAL JOINT AND TIE BARS ALONG LANE EDGE WHEN CONCRETE PANEL WIDTH EXCEEDS THE MAXIMUM WIDTH PER TABLE BELOW. LONGITUDINAL JOINT(S) ARE NOT ALLOWED WITHIN TRAFFIC LANES AND BIKE LANES. LONGITUDINAL JOINT MAY BE SAWED.
- ⑨ CONCRETE CURB AND GUTTER 4-INCH SLOPED 30-INCH TYPE "R" AND "T" = 17 INCHES  
CONCRETE CURB AND GUTTER 4-INCH SLOPED 36-INCH TYPE "R" AND "T" = 23 INCHES



**END SECTION CURB AND GUTTER**



**DETAIL OF CURB AND GUTTER AT INLETS**

(TYPICAL H INLET COVER SHOWN)

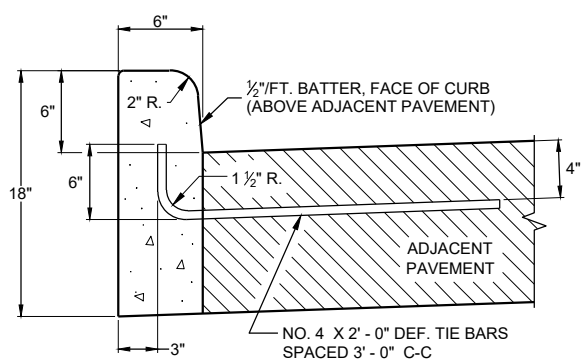
**GENERAL NOTES**

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

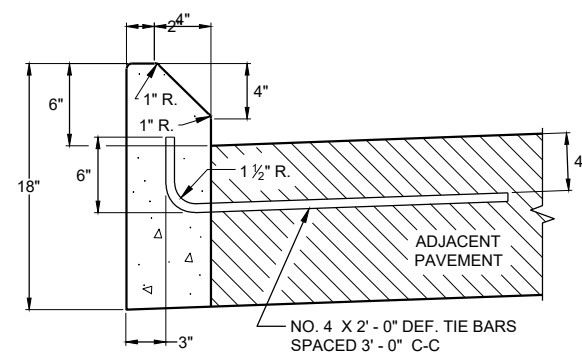
PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

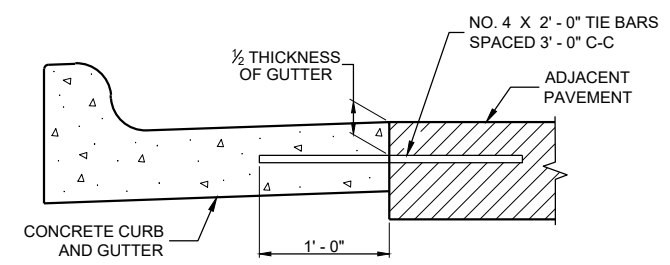
- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑩ REFER TO SDD 08D18 AND 08D19 FOR ADDITIONAL DRIVEWAY ENTRANCE CURB DETAILS.
- ⑪ PLACE 1" THICK EXPANSION JOINT MATERIAL BETWEEN VERTICAL FACE CURB TYPES EXTENDING FROM THE TOP OF CURB TO 1 INCH BELOW THE ADJOINING CONCRETE SURFACE. RIGID CONCRETE STRUCTURES INCLUDE RAISED CONCRETE MEDIANS, CONCRETE SAFETY ISLANDS, SPLITTER ISLANDS, OR LOCATIONS IDENTIFIED ON THE PLANS.



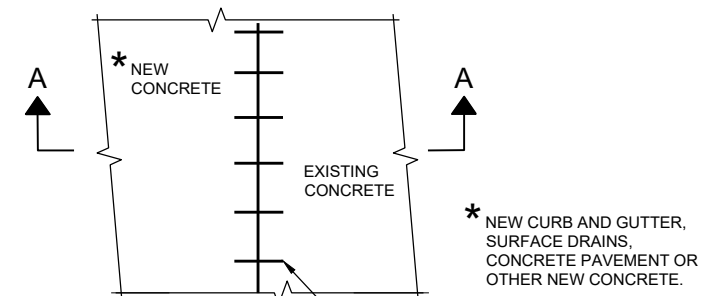
**TYPES A<sup>①</sup> & D**



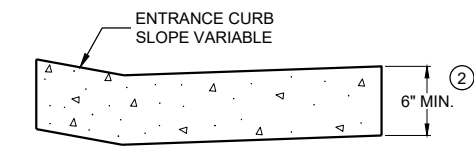
**TYPES G<sup>①</sup> & J  
CONCRETE CURB**



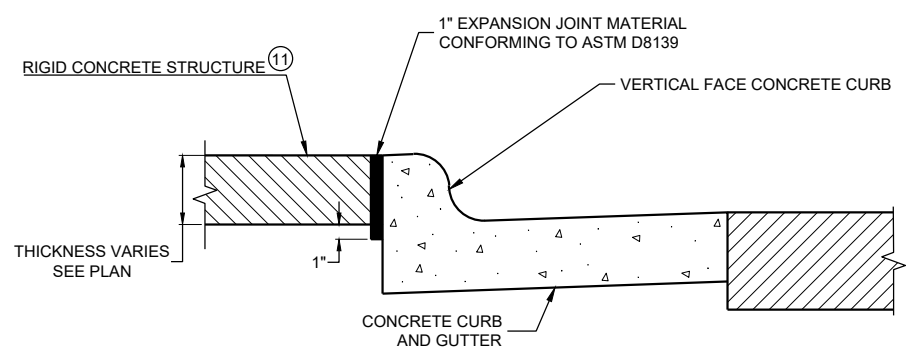
**TYPICAL TIE BAR LOCATION<sup>①</sup>**



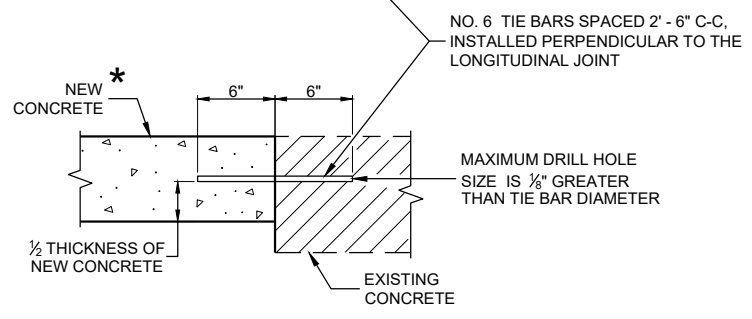
**PLAN VIEW**



**DRIVEWAY ENTRANCE CURB<sup>⑩</sup>  
(WHEN DIRECTED BY THE ENGINEER)**

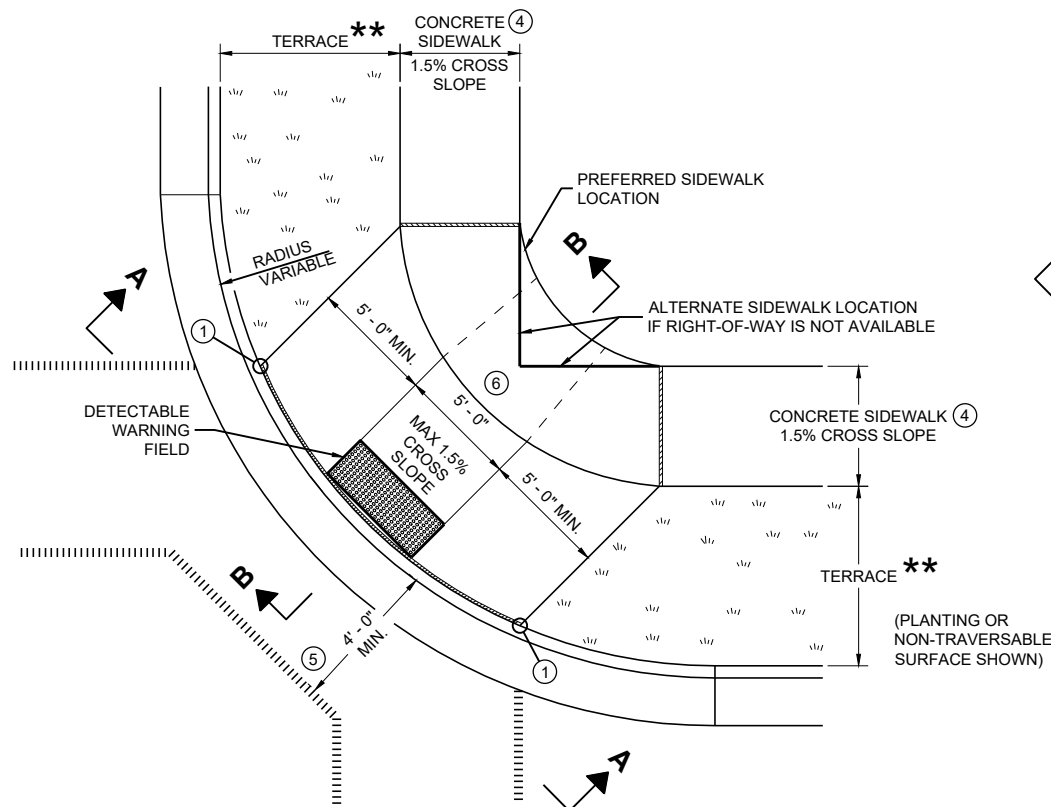


**EXPANSION JOINT DETAIL FOR VERTICAL CURB ABUTTING A RIGID STRUCTURE<sup>⑪</sup>**

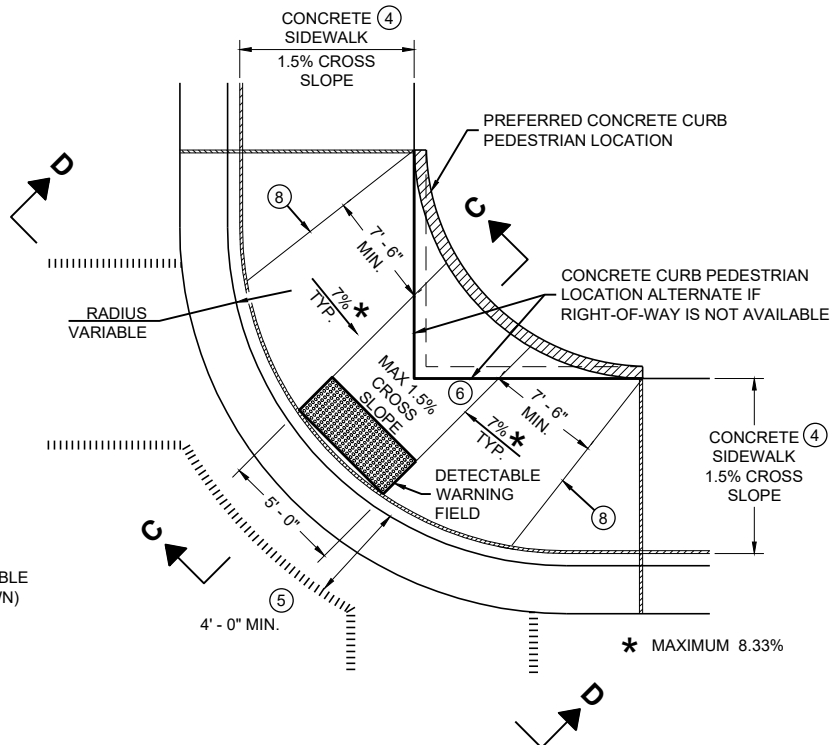


**SECTION A - A  
TIE BARS DRILLED INTO EXISTING PAVEMENT**

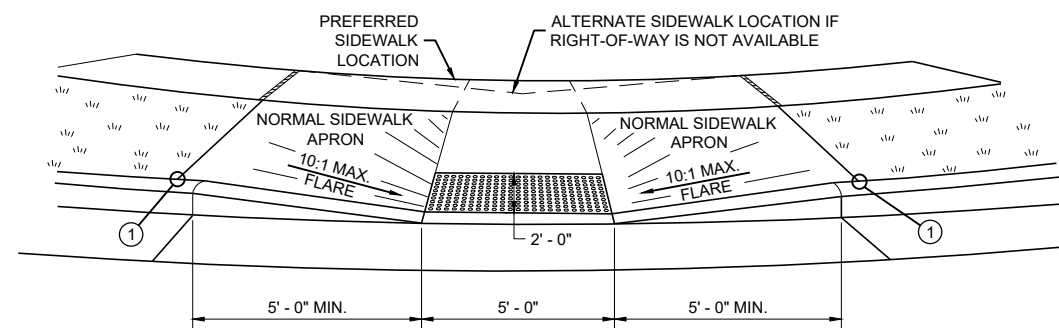
<b>CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS</b>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED DATE	/S/ Rodney Taylor ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA	



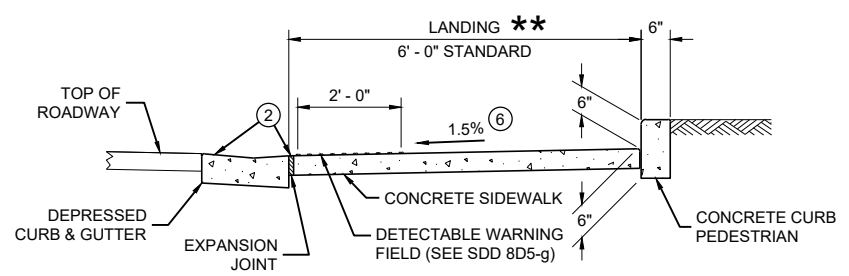
**PLAN VIEW  
CURB RAMP TYPE 1  
(CENTER OF CORNER RADIUS)**



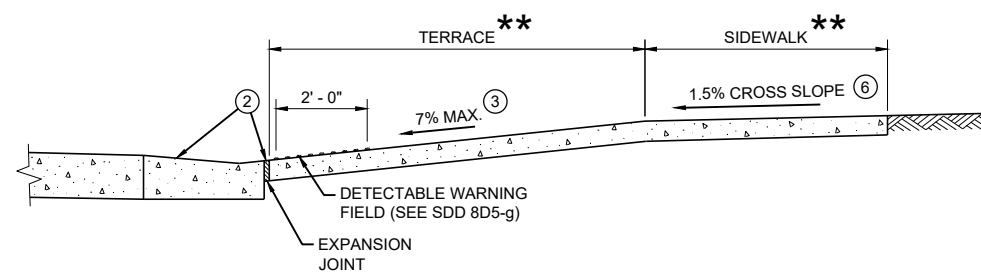
**PLAN VIEW  
CURB RAMP TYPE 1 - A  
(NO TERRACE)**



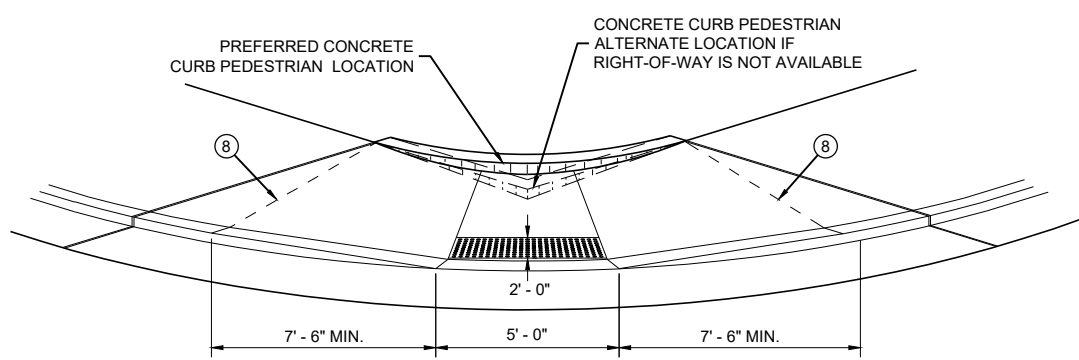
**VIEW A - A FOR TYPE 1**



**SECTION C - C FOR TYPE 1 - A**



**SECTION B - B FOR TYPE 1**



**VIEW D - D FOR TYPE 1 - A**

**GENERAL NOTES**

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- WHEN NECESSARY, THE SIDEWALK ELEVATION MAY BE LOWERED TO MEET THE HIGH POINT ON THE RAMP.
- TYPE 1 CURB RAMPS SHALL HAVE A NORMAL SIDEWALK APRON AND CURB ON BOTH SIDES OF RAMP.
- DETECTABLE WARNING FIELD SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS "CURB RAMP DETECTABLE WARNING FIELD". THE CONCRETE PEDESTRIAN CURB, IF NEEDED, SHALL BE MEASURED AND PAID BY THE LINEAR FOOT AS "CONCRETE CURB PEDESTRIAN". CONCRETE SIDEWALK IN THE CURB RAMP AREA SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS CONCRETE SIDEWALK, INCLUDING THE AREA UNDER THE DETECTABLE WARNING FIELD.
- SELECT CURB RAMP DETECTABLE WARNING FIELD MATERIALS AND DEVICES FROM THE DEPARTMENT'S APPROVED MATERIALS LIST. THE COLOR OF THE DETECTABLE WARNING FIELD IS SPECIFIED ELSEWHERE AND IS INCIDENTAL TO THE BID ITEM OF "CURB RAMP DETECTABLE WARNING FIELD"
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- SURFACE TEXTURE OF THE RAMP SHALL BE OBTAINED BY COARSE BROOMING TRANSVERSE TO THE SLOPE OF THE RAMP.
- ① THIS POINT IS AN EXTENSION OF OUTSIDE EDGE OF APPROACHING SIDEWALK WHERE IT MEETS THE BACK OF CONCRETE CURB. POINT LOCATION MAY BE ADJUSTED TO ALIGN WITH BEGINNING OF FULL-HEIGHT CURB IF THIS DISTANCE IS SHORT.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ MAXIMUM 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑤ PROVIDE A LEVEL LANDING IN THE STREET AND GUTTER AREA (2% MAXIMUM SLOPE IN ANY DIRECTION). WHEN THE GUTTER SLOPE EXCEEDS 2%, CONSTRUCT THE LEVEL LANDING IN THE STREET AREA. 4 FOOT WIDTH IS MEASURED FROM THE FLANGE LINE
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

**LEGEND**

- 1/2" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)

**CURB RAMPS  
TYPE 1 AND 1-A**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

6

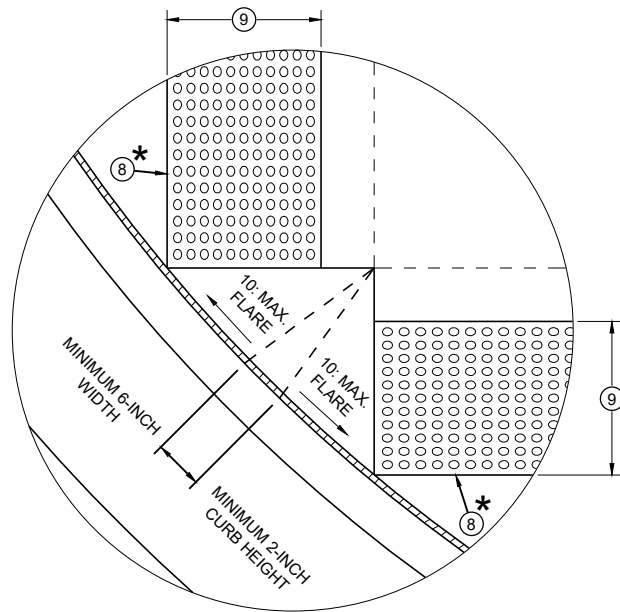
6

SDD 08D05-21a

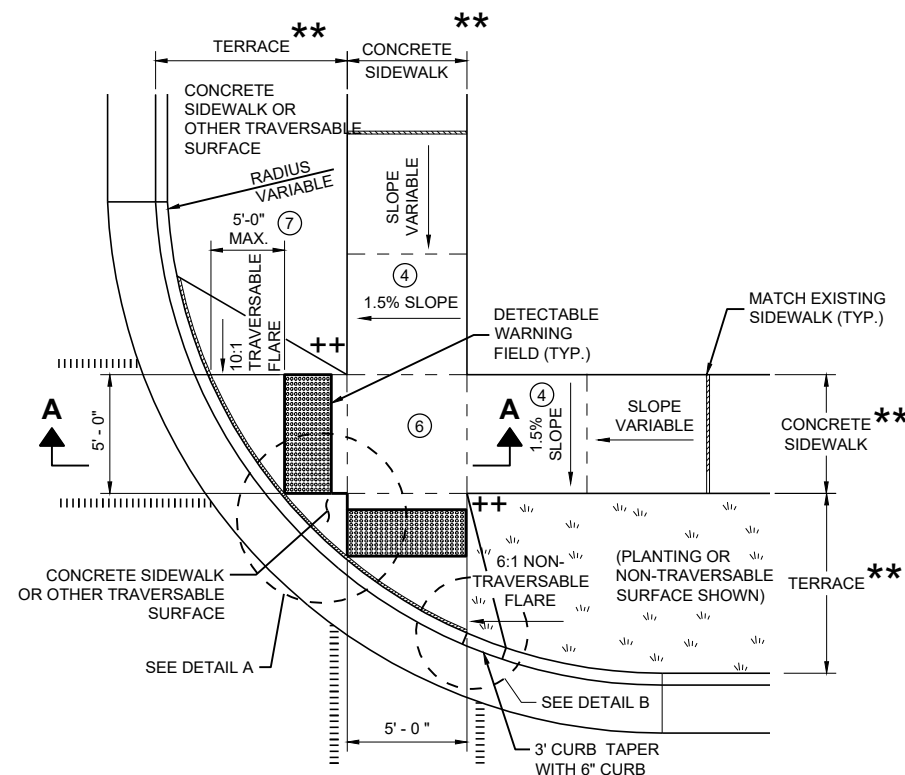
SDD 08D05-21a

\*\* WIDTH SHOWN ELSEWHERE IN THE PLANS

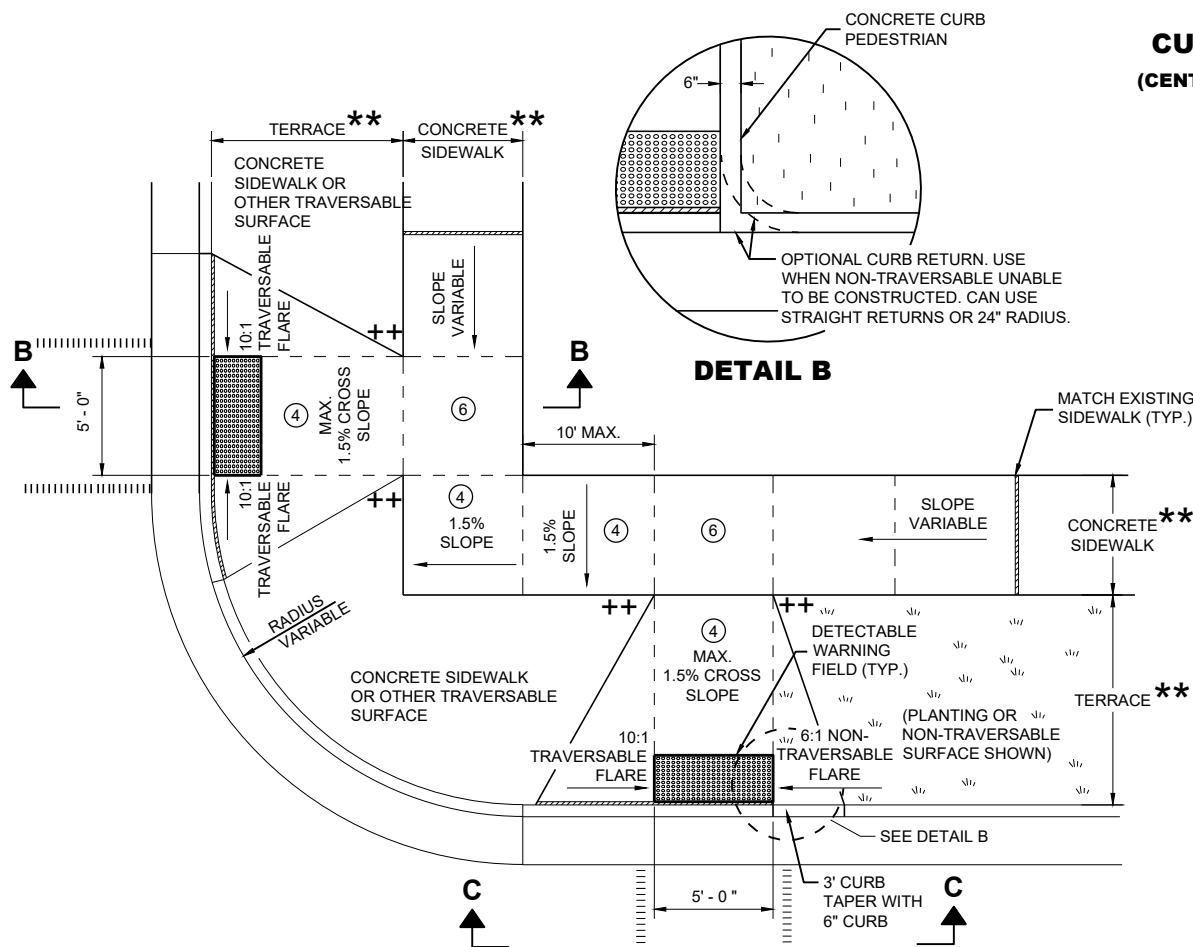




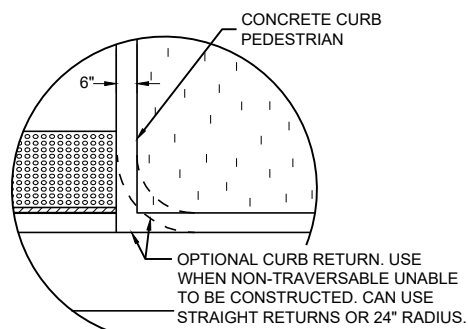
**DETAIL A**



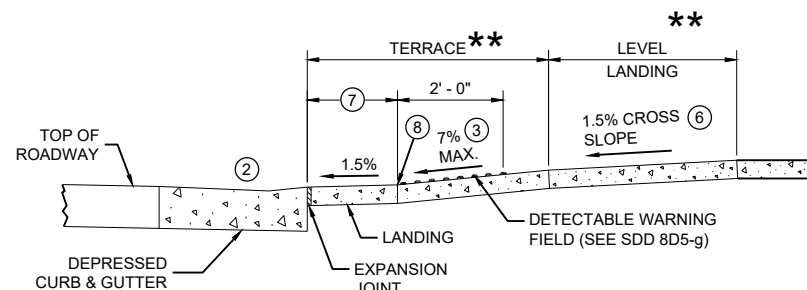
**PLAN VIEW  
CURB RAMP TYPE 2  
(CENTER OF CORNER RADIUS)**



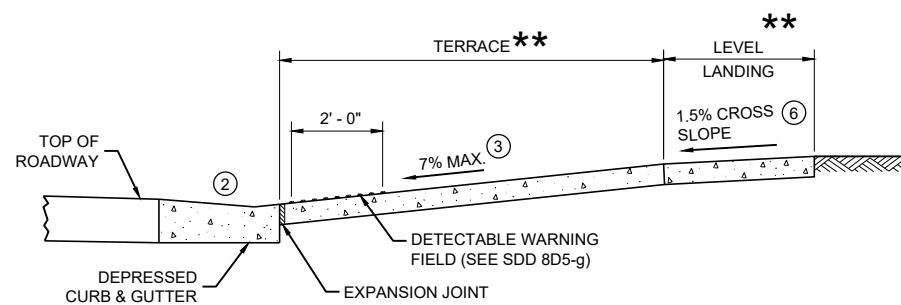
**PLAN VIEW  
CURB RAMP TYPE 3  
(OUTSIDE OF CROSSWALK AREA)**



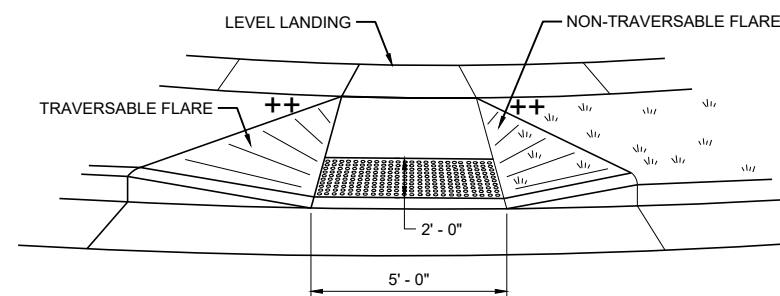
**DETAIL B**



**SECTION A - A FOR TYPE 2**



**SECTION B - B FOR TYPE 3**



**VIEW C - C FOR TYPE 3**

**GENERAL NOTES**

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE (2.67% OR LESS) AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET X 5 FEET.
- ⑦ WHEN GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑨ WHEN DISTANCE IS LESS THAN 6' - 0", IT MAY BE DIFFICULT TO ACHIEVE A 7% SLOPE OR FLATTER ALONG THE RAMP. REDUCE CURB HEIGHT IN TRIANGLE AREA TO ACHIEVE 7% SLOPE OR FLATTER ON RAMP. CONSTRUCT 2-INCH MINIMUM CURB HEIGHT BETWEEN 10:1 FLARES.

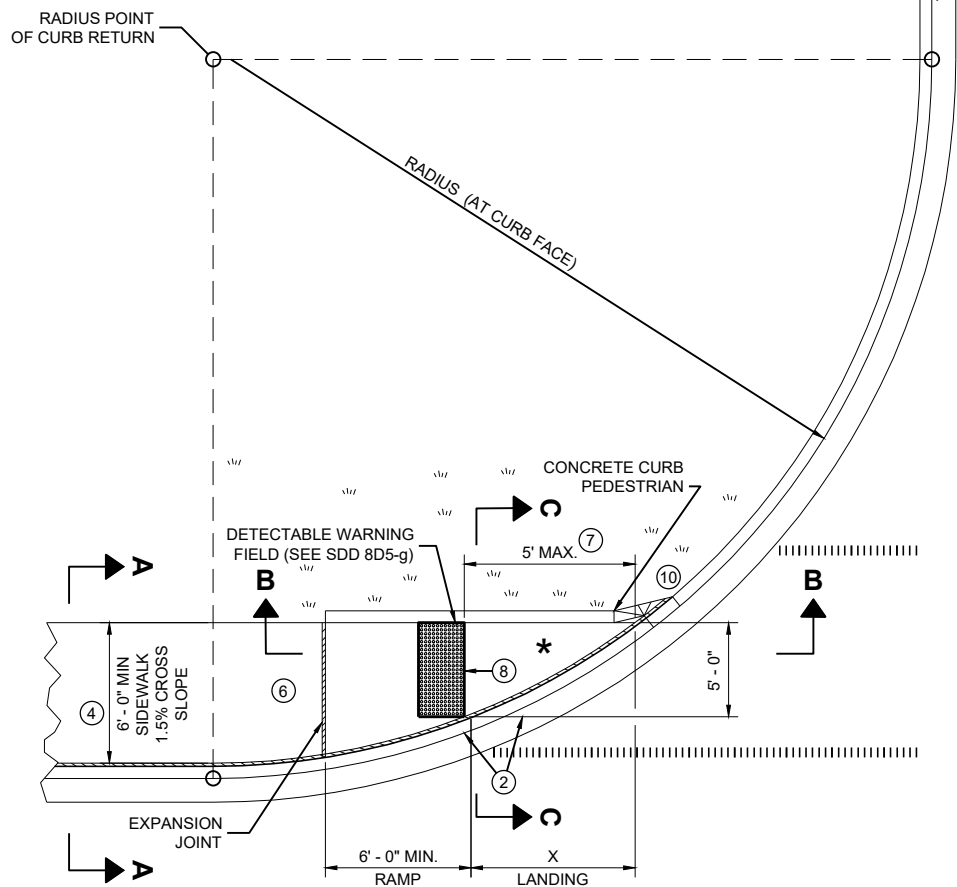
- \* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK
- \*\* WIDTH SHOWN ELSEWHERE IN THE PLANS
- ++ CONSTRUCT 6" WEDGE TO AVOID CONCRETE BREAKAGE

**LEGEND**

- 1/2" EXPANSION JOINT SIDEWALK
- - - - CONTRACTION JOINT SIDEWALK
- |||||| PAVEMENT MARKING CROSSWALK (WHITE)

**CURB RAMPS  
TYPE 2 AND 3**

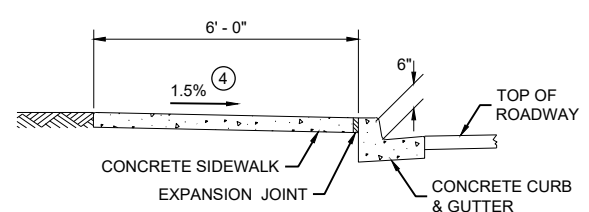
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION



**PLAN VIEW  
CURB RAMP TYPE 4A**

RADIUS (AT CURB FACE)	X
10 FEET	4' - 7"

INTERMEDIATE RADII CAN BE INTERPOLATED



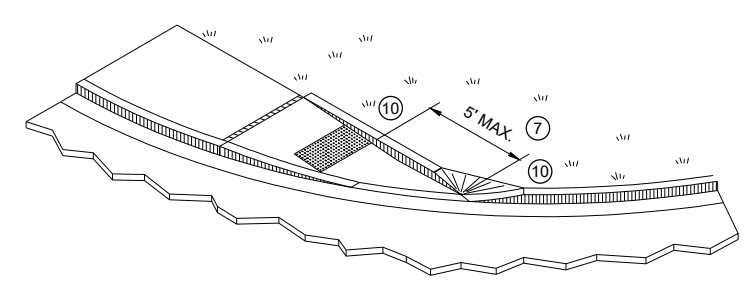
**SECTION A - A FOR TYPE 4A**

**GENERAL NOTES**

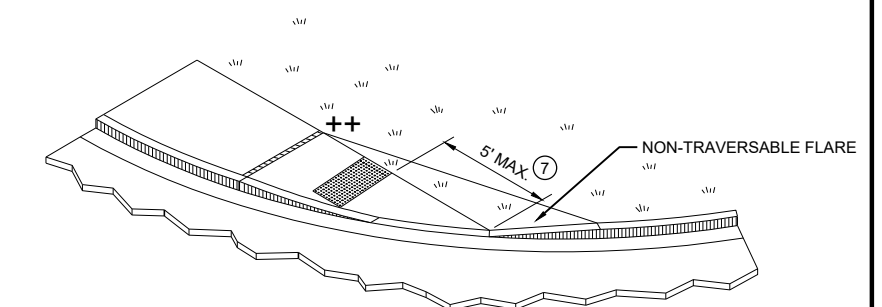
- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑦ WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑩ INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

**LEGEND**

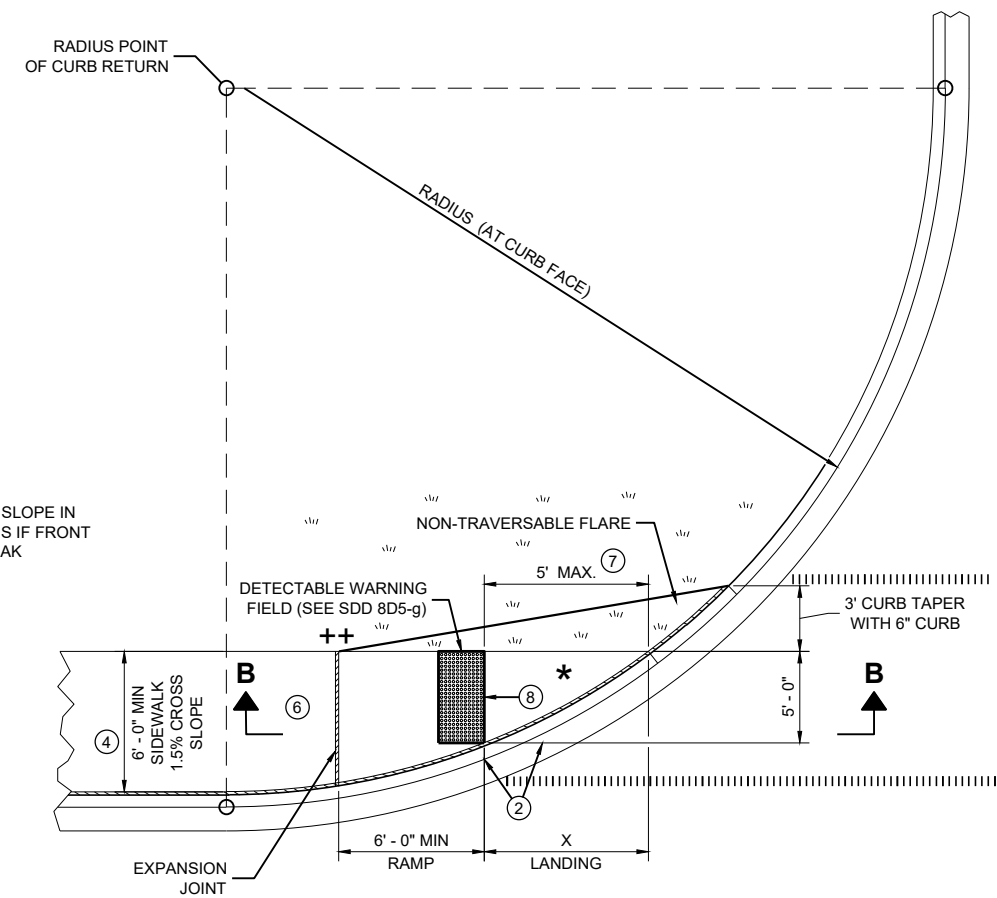
- 1/2" EXPANSION JOINT SIDEWALK
- - - - - CONTRACTION JOINT SIDEWALK
- ||||| PAVEMENT MARKING CROSSWALK (WHITE)



**ISOMETRIC VIEW FOR TYPE 4A**



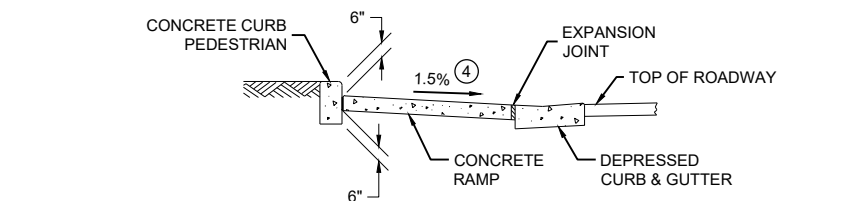
**ISOMETRIC VIEW FOR TYPE 4A1**



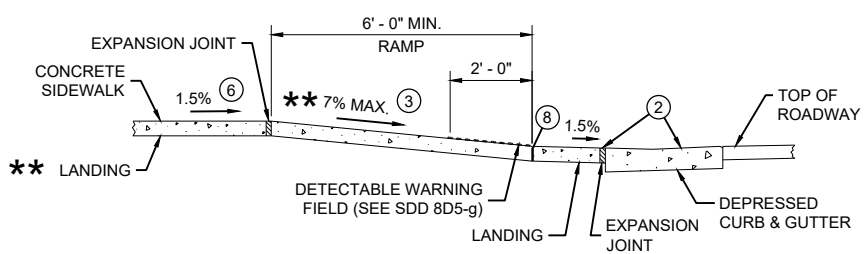
**PLAN VIEW  
CURB RAMP TYPE 4A1**

\* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IF FRONT OF GRADE BREAK

++ CONSTRUCT 6" WEDGE TO AVOID CONCRETE BREAKAGE



**SECTION C - C FOR TYPE 4A**

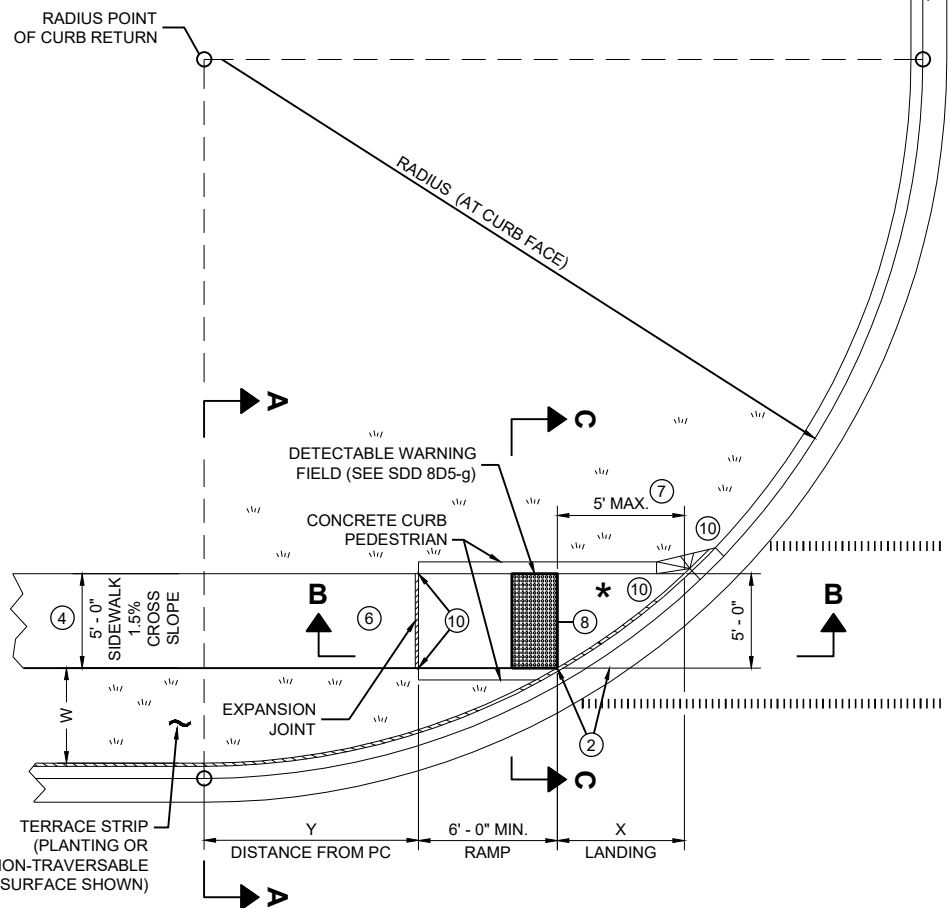


**SECTION B - B FOR  
TYPE 4A AND TYPE 4A1**

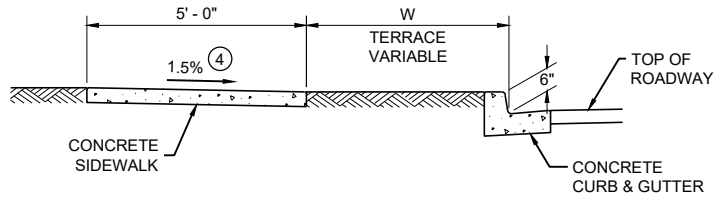
\*\* IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

**CURB RAMPS  
TYPE 4A AND 4A1**

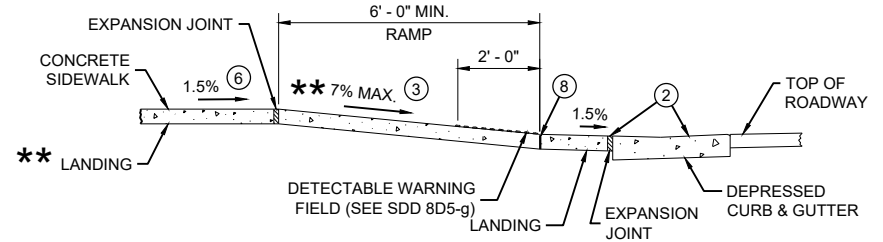
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION



**PLAN VIEW CURB RAMP TYPE 4B**



**SECTION A - A FOR TYPE 4B**

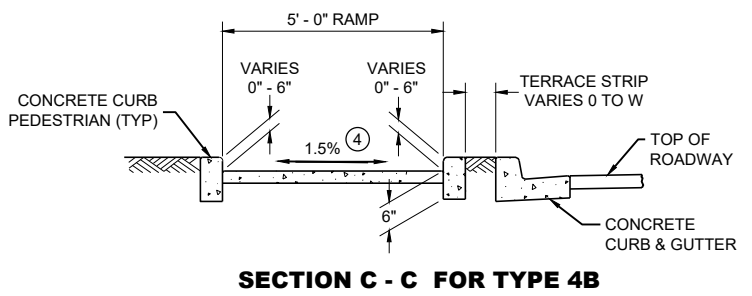


**SECTION B - B FOR TYPE 4B AND TYPE 4B1**

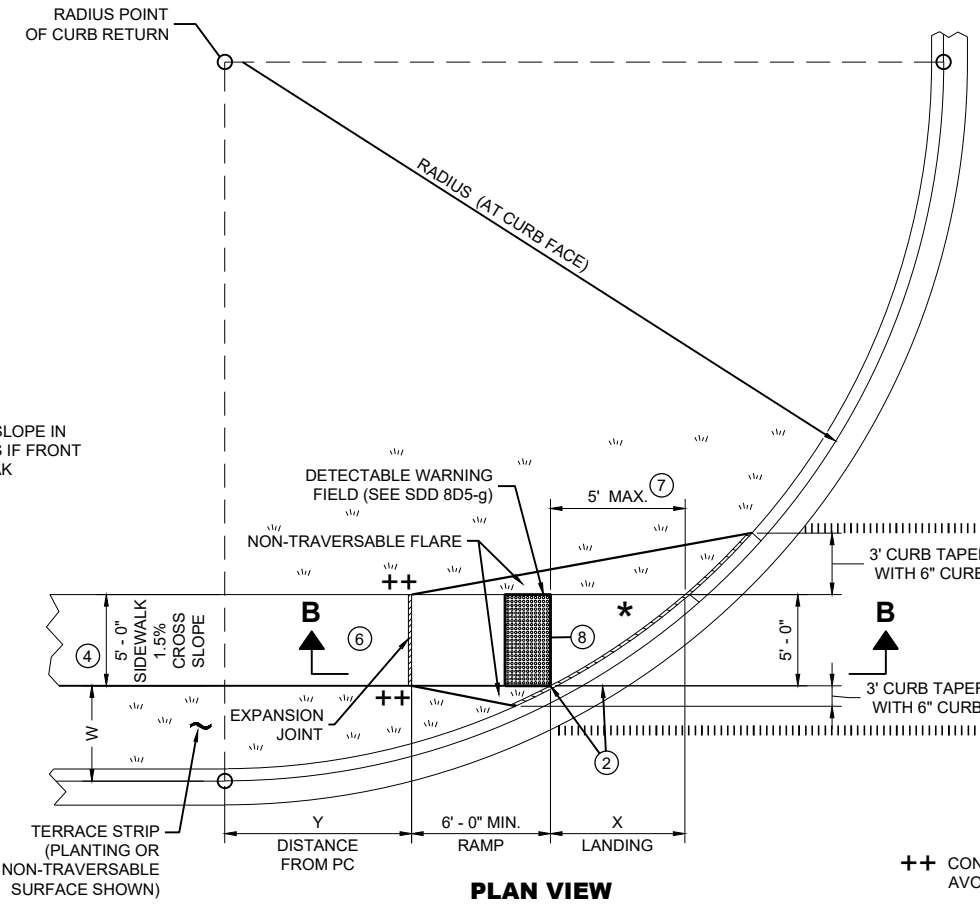
\*\* IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

RADIUS (AT CURB FACE)	W = 3' - 0"		W = 4' - 0"		W = 5' - 0"		W = 6' - 0"		W = 7' - 0"		W = 8' - 0"		W = 9' - 0"		W = 10' - 0"	
	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y
10 FEET	2' - 10 1/4"	0' - 5"	2' - 1"	1' - 4 1/2"	1' - 5"	2' - 1"	0' - 10"	2' - 7 1/2"	0' - 3 1/4"	3' - 0 1/4"						
15 FEET	4' - 6 3/4"	2' - 1 3/4"	3' - 9"	3' - 5 3/4"	3' - 1 1/4"	4' - 6"	2' - 6 3/4"	5' - 4 1/2"	2' - 1"	6' - 1"	1' - 8"	6' - 8 1/2"	1' - 3 1/4"	7' - 2 1/2"	0' - 10 3/4"	7' - 7 1/4"
20 FEET			4' - 11 1/2"	5' - 1 3/4"	4' - 3 1/4"	6' - 5 1/2"	3' - 8 3/4"	7' - 7"	3' - 3"	8' - 6 1/2"	2' - 10"	9' - 4 1/2"	2' - 5 1/2"	10' - 1 1/4"	2' - 1 1/4"	10' - 9"
30 FEET									4' - 10 3/4"	12' - 5 3/4"	4' - 5 1/2"	13' - 7 3/4"	4' - 0 3/4"	14' - 8 1/2"	3' - 8 1/2"	15' - 8 1/4"
40 FEET															4' - 10 3/4"	19' - 8 1/4"

INTERMEDIATE RADII CAN BE INTERPOLATED  
 DIMENSION "Y" IS CALCULATED BASED ON 6'-0" RAMP LENGTH  
 DIMENSION "X" IS CALCULATED BASED ON 5'-0" SIDEWALK WIDTH

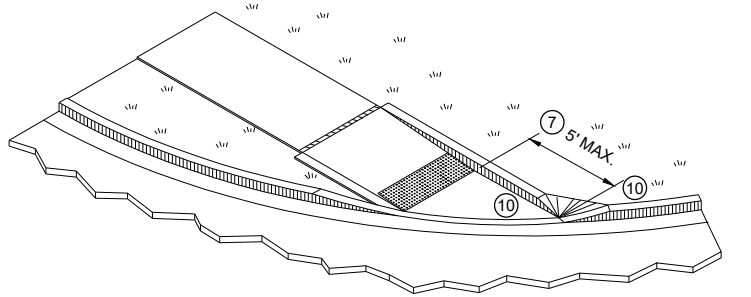


**SECTION C - C FOR TYPE 4B**

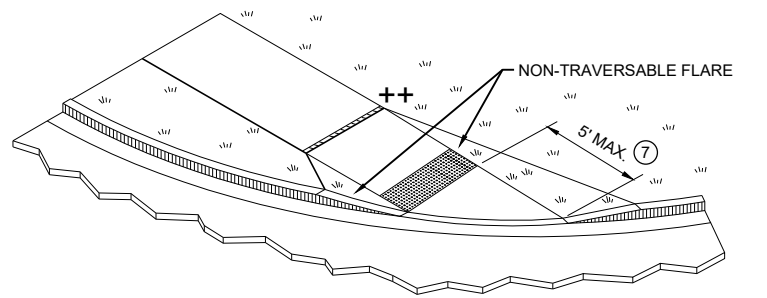


**PLAN VIEW CURB RAMP TYPE 4B1**

++ CONSTRUCT 6" WEDGE TO AVOID CONCRETE BREAKAGE



**ISOMETRIC VIEW FOR TYPE 4B**



**ISOMETRIC VIEW FOR TYPE 4B1**

**LEGEND**

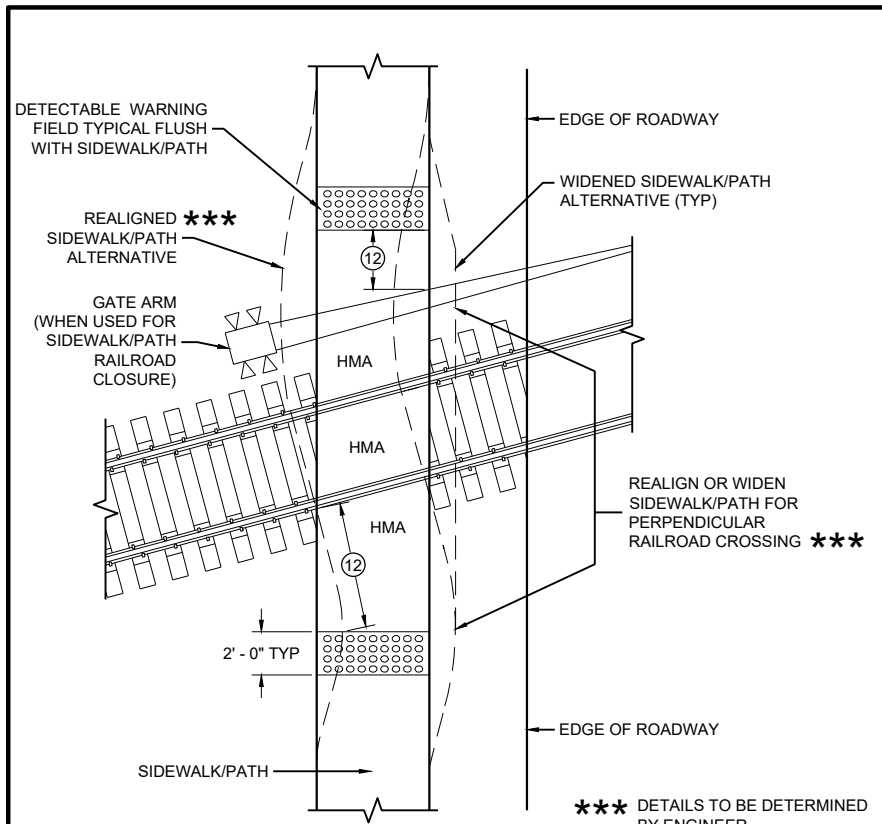
- 1/2" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT SIDEWALK
- PAVEMENT MARKING CROSSWALK (WHITE)

**GENERAL NOTES**

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- (2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4" - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- (3) AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- (4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- (6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- (7) WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- (8) PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- (10) INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

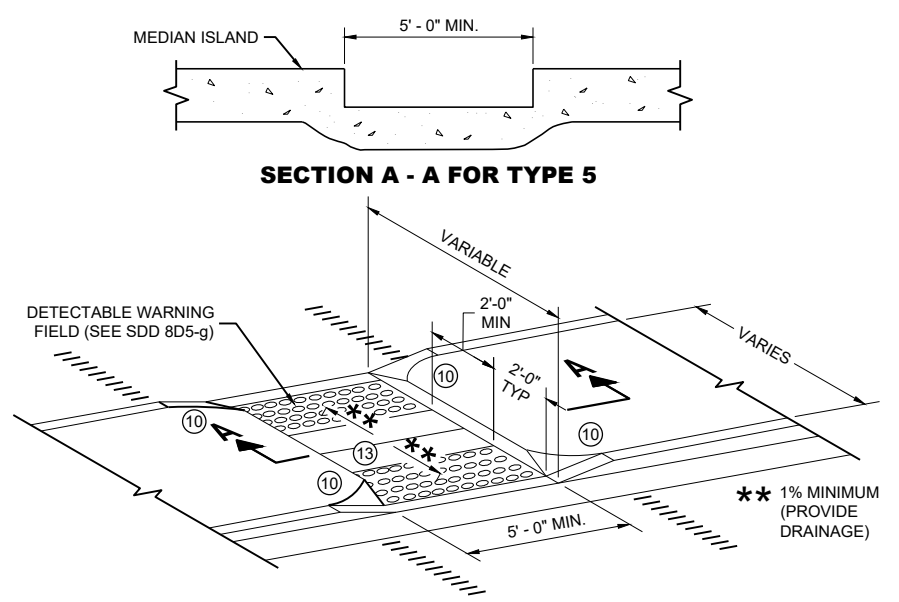
**CURB RAMPS TYPE 4B AND 4B1**

STATE OF WISCONSIN  
 DEPARTMENT OF TRANSPORTATION



**CURB RAMP TYPE 8**  
**DETECTABLE WARNINGS**  
**FOR SIDEWALKS OR SHARED USE PATHS**  
**AT RAILROAD CROSSINGS**

\*\*\* DETAILS TO BE DETERMINED BY ENGINEER



**SECTION A - A FOR TYPE 5**

**CURB RAMP TYPE 5**  
**MEDIAN ISLAND**  
**NON-ELEVATED PEDESTRIAN CROSSING**

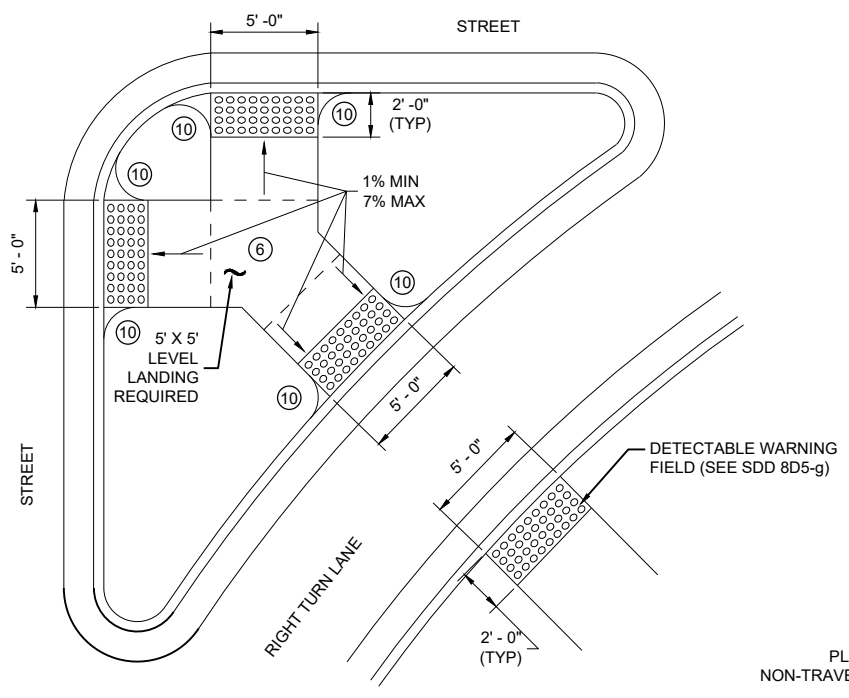
**GENERAL NOTES**

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑩ INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.
- ⑪ SLOPE SIDEWALK TOWARD LANDING AS SHOWN WHERE THERE IS NO TERRACE OR WHERE THE TERRACE WIDTH IS LESS THAN 6 FEET WIDE.
- ⑫ THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO A RAILROAD CROSSING SHALL BE 1.5 FEET ±0.1' FROM THE FACE OF THE GATE ARM IF THE GATE ARM EXTENDS ACROSS THE SIDEWALK/PATH. WHERE THERE IS NO PEDESTRIAN GATE, THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO THE RAILROAD TRACK IS 15 FEET MAXIMUM AND 12 FEET MINIMUM, 15 FEET TYPICAL FROM THE NEAREST RAIL.
- ⑬ DO NOT INSTALL DETECTABLE WARNING FIELDS AT THE EDGES OF STREET-LEVEL PEDESTRIAN REFUGE ISLANDS IF A MINIMUM 2 FOOT CONCRETE SURFACE WITHOUT DETECTABLE WARNINGS (MEASURED IN THE DIRECTION OF PEDESTRIAN TRAVEL) CANNOT BE ACHIEVED.

**LEGEND**

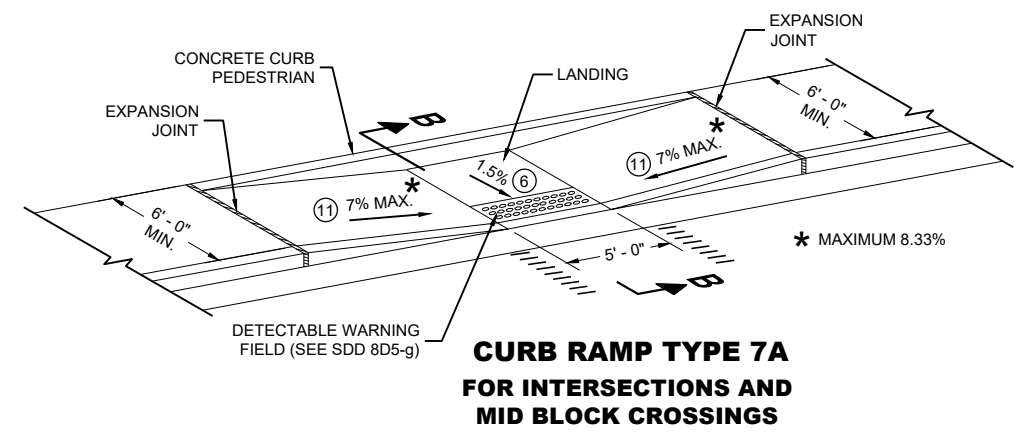
- ===== 1/2" EXPANSION JOINT SIDEWALK
- - - - - CONTRACTION JOINT FIELD LOCATED
- ||||||| PAVEMENT MARKING CROSSWALK (WHITE)

6

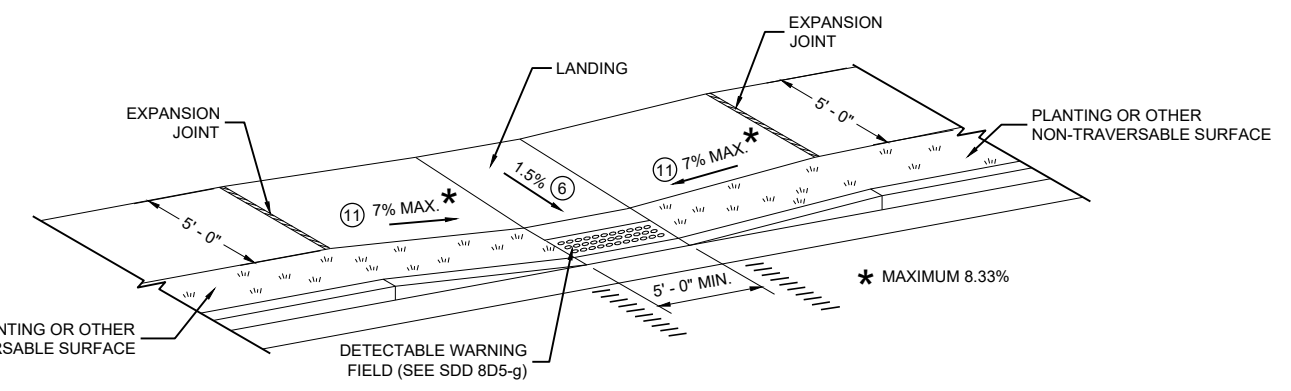


**CURB RAMP TYPE 6**  
**DETECTABLE WARNING AT ISLANDS**

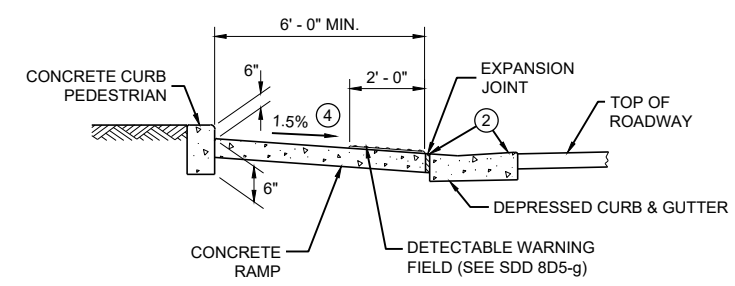
REFER TO GENERAL NOTES ② AND ③ FOR ALL ISLAND CURB RAMPS



**CURB RAMP TYPE 7A**  
**FOR INTERSECTIONS AND**  
**MID BLOCK CROSSINGS**



**CURB RAMP TYPE 7B**  
**FOR INTERSECTIONS AND**  
**MID BLOCK CROSSINGS**



**SECTION B - B FOR TYPE 7A**

6

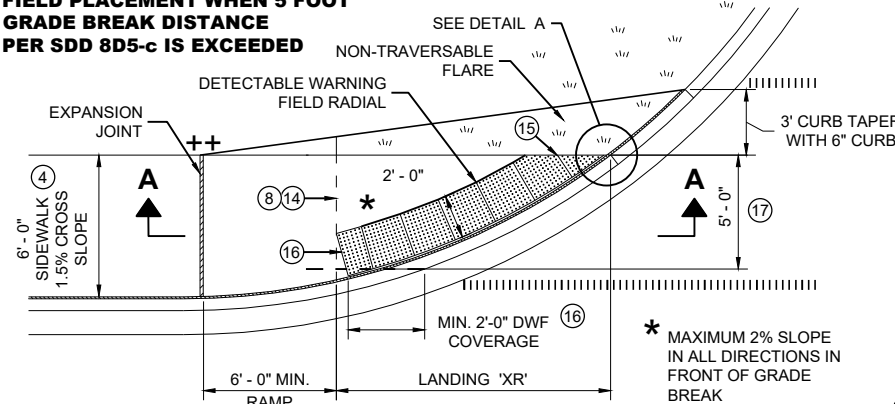
SDD 08D05-21e

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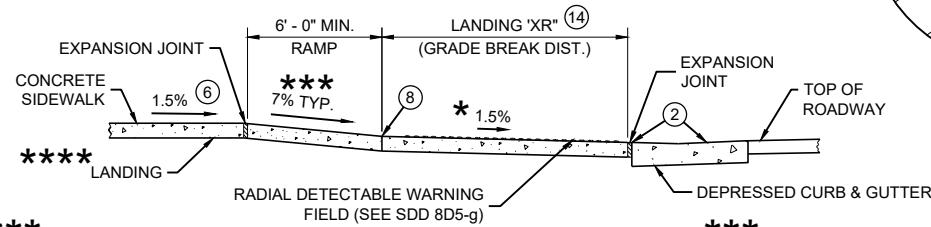
**CURB RAMPS**  
**TYPE 5, 6, 7A, 7B & 8**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

**RADIAL DETECTABLE WARNING  
FIELD PLACEMENT WHEN 5 FOOT  
GRADE BREAK DISTANCE  
PER SDD 8D5-c IS EXCEEDED**



**PLAN VIEW  
CURB RAMP TYPE 4A1  
(GRADE BREAK DISTANCE GREATER THAN 5 FEET)**



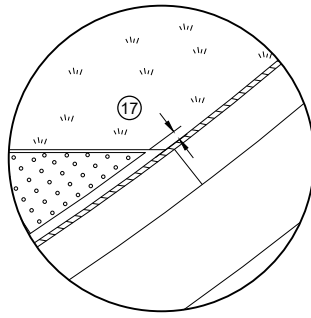
**SECTION A - A FOR TYPE 4A1**

\*\*\*\* IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

\*\*\* MAXIMUM 8.33%

**LEGEND**

- 1/2" EXPANSION JOINT SIDEWALK
- - - - - CONTRACTION JOINT SIDEWALK
- ||||| PAVEMENT MARKING CROSSWALK (WHITE)

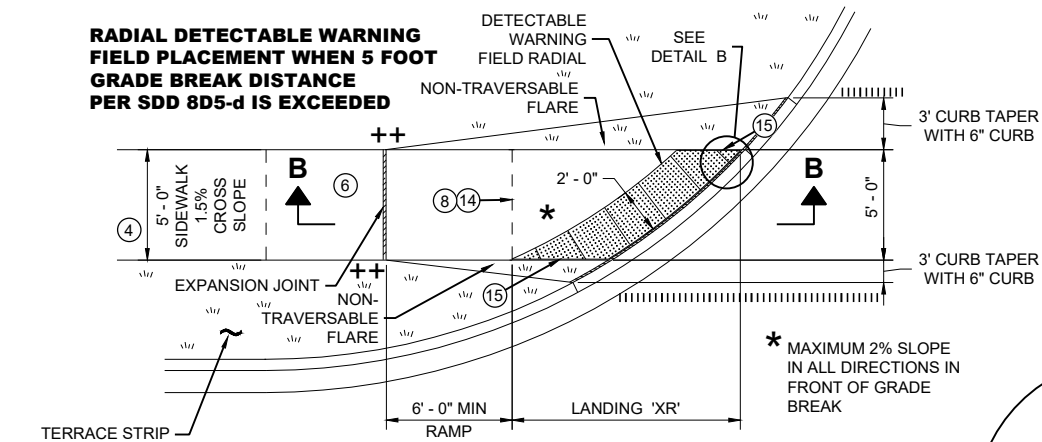


**DETAIL A**

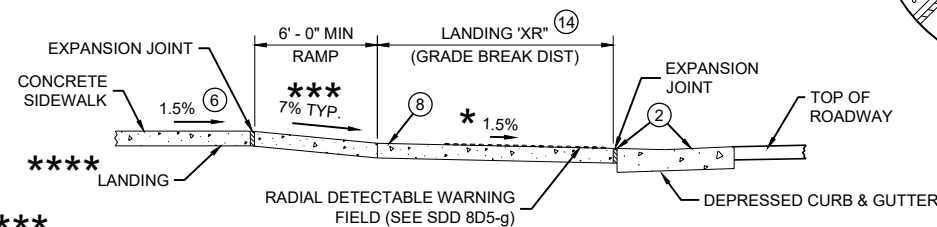
**GENERAL NOTES**

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- APPLY RADIAL DETECTABLE WARNING PLACEMENT SIMILARLY FOR TYPE 4A AND 4A1 CURB RAMPS AND SIMILARLY FOR TYPE 4B AND 4B1 CURB RAMPS. TYPE 4A AND 4B CURB RAMPS ARE NOT SHOWN.
- REFER TO SDD 8D5-g FOR ADDITIONAL RADIAL PLATE REQUIREMENTS.
- FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FIELD ARE PROHIBITED.
- DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.
- (2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4" INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- (3) AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- (4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- (6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET BY 5 FEET.
- (8) PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- (14) CONSULT ENGINEER IF GRADE BREAK LOCATION (END OF LANDING DIMENSION "XR") REQUIRES FIELD ADJUSTMENT WHEN ESTABLISHING FINAL RADIAL DETECTABLE WARNING FIELD LOCATION.
- (15) FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN 1/2" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.
- (16) USE 1' X 2" RECTANGULAR END PLATE AT END OF TYPE 4A1 RAMP AND PROVIDE MINIMUM 2' - 0" DETECTABLE WARNING FIELD COVERAGE (IN DIRECTION OF PEDESTRIAN TRAVEL) ALONG THE ENTIRE CURB RAMP WIDTH.
- (17) A MAXIMUM 3 INCH CONCRETE BORDER WIDTH IS ALLOWABLE IN FRONT OF RADIAL DETECTABLE WARNING FIELD FOR CONSTRUCTABILITY PURPOSES. CONCRETE BORDER WIDTH MAY VARY UP TO 1 INCH.

**RADIAL DETECTABLE WARNING  
FIELD PLACEMENT WHEN 5 FOOT  
GRADE BREAK DISTANCE  
PER SDD 8D5-d IS EXCEEDED**



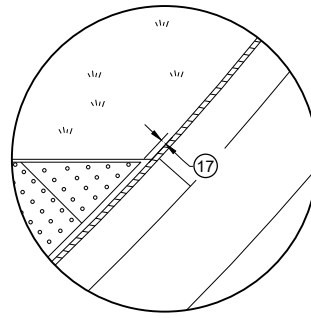
**PLAN VIEW  
CURB RAMP TYPE 4B1  
(GRADE BREAK DISTANCE GREATER THAN 5 FEET)**



**SECTION B - B FOR TYPE 4B1**

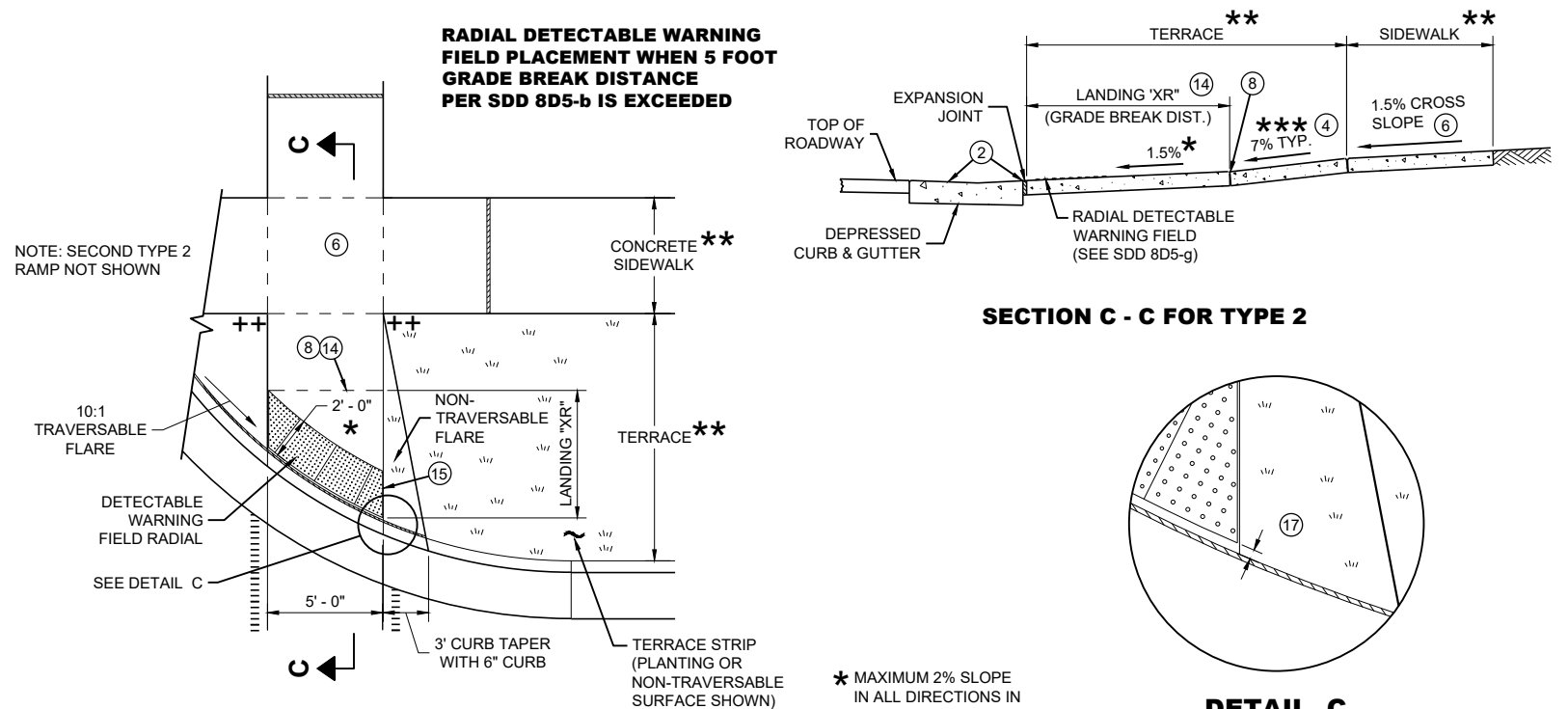
\*\*\*\* IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

\*\*\* MAXIMUM 8.33%



**DETAIL B**

**RADIAL DETECTABLE WARNING  
FIELD PLACEMENT WHEN 5 FOOT  
GRADE BREAK DISTANCE  
PER SDD 8D5-b IS EXCEEDED**



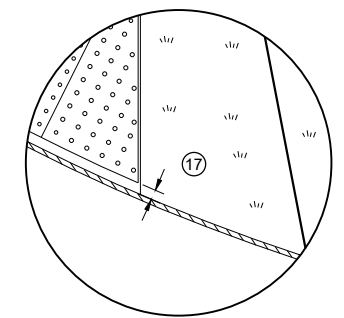
**PLAN VIEW  
CURB RAMP TYPE 2  
(GRADE BREAK DISTANCE GREATER THAN 5 FEET)  
(ON LINE WITH SIDEWALK)**

\* MAXIMUM 2% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK

\*\* WIDTH SHOWN ELSEWHERE IN THE PLANS

\*\*\* MAXIMUM 8.33%

++ CONSTRUCT 6" WEDGE TO AVOID CONCRETE BREAKAGE



**DETAIL C**

**CURB RAMPS  
RADIAL DETECTABLE WARNING**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

6

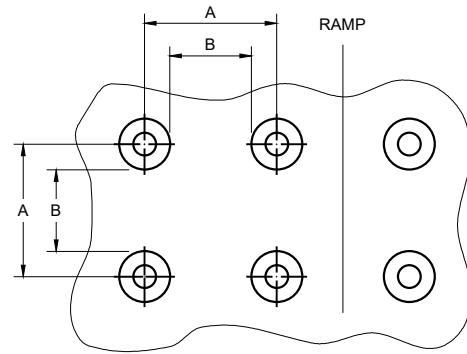
6

SDD 08D05-21f

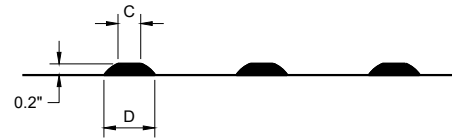
SDD 08D05-21f

	MIN.	MAX.
<b>A</b>	1.6"	2.4"
<b>B</b>	0.65"	1.5"
<b>C</b>	*	*
<b>D</b>	0.9"	1.4"

\* THE C DIMENSION IS 50% TO 65% OF THE D DIMENSION.

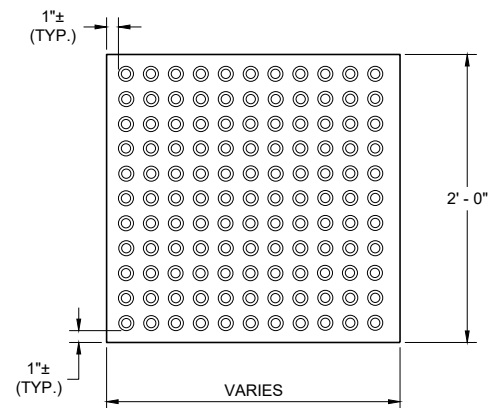


**PLAN VIEW**

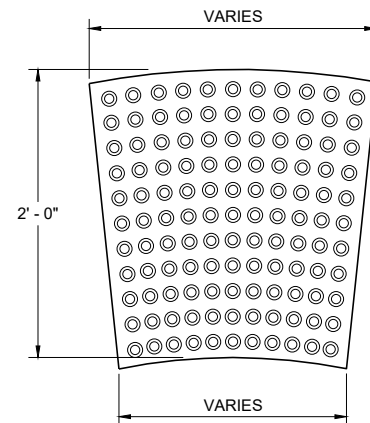


**ELEVATION VIEW**

**TRUNCATED DOMES  
DETECTABLE WARNING PATTERN DETAIL**

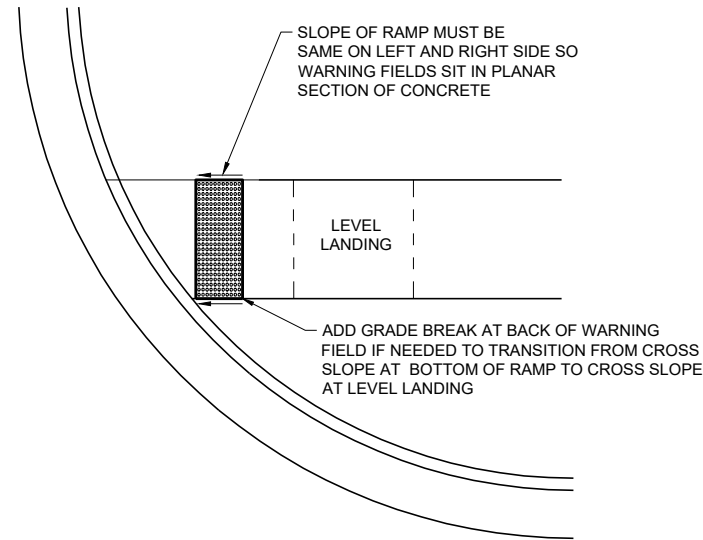


**RECTANGULAR  
PLATES**



**RADIAL  
PLATES**

**PLAN VIEW  
DETECTABLE WARNING FIELDS (TYPICAL)**

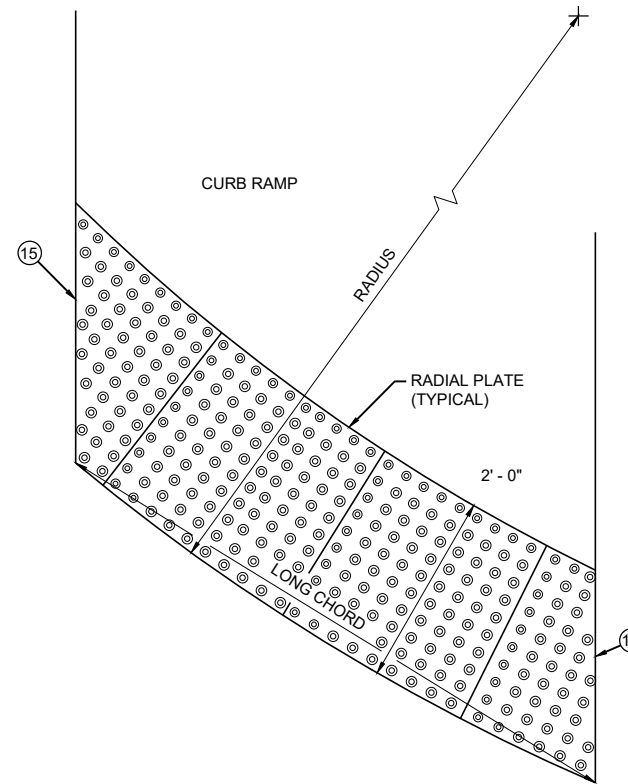


**DETECTABLE WARNING FIELD  
PLANAR INSTALLATION**

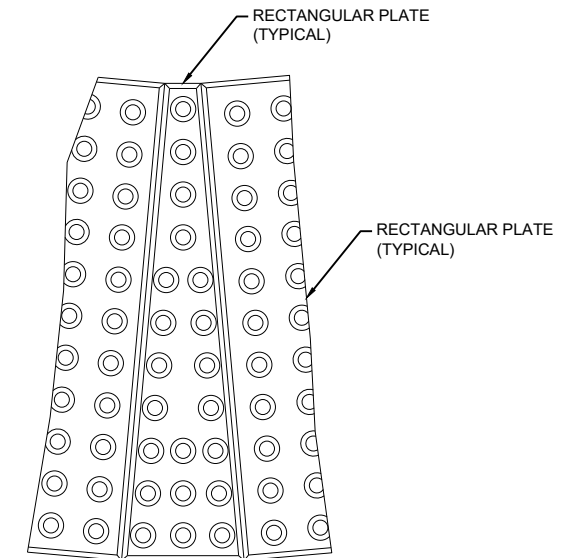
**GENERAL NOTES**

- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AT A CURB RAMP SHALL BE FROM THE SAME MANUFACTURER.
- PLACE ALL DETECTABLE WARNING FIELD SYSTEMS IN ACCORDANCE TO THE MANUFACTURER'S RECOMMENDATION.
- FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FIELD ARE PROHIBITED.
- DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.
- FOR RADIAL DETECTABLE WARNING FIELD APPLICATIONS WHERE STANDARD RADIAL PLATES ARE NOT AVAILABLE AT AN INTERSECTION CURB RADIUS, A COMBINATION OF SQUARE OR RECTANGULAR PLATES AND RADIAL PLATES MAY BE USED TO FORM RADIAL CONFIGURATION. RADIAL WEDGE PLATES IN COMBINATION WITH SQUARE PLATES ARE ALSO ACCEPTABLE. FOLLOW MANUFACTURER'S RECOMMENDATIONS.
- REFER TO CONTRACT AND STANDARD SPECIFICATIONS FOR FIELD CUTTING REQUIREMENTS.
- DO NOT EMBED IN CONCRETE ANY FIELD-CUT PLATES WITH CUT EDGES SHORTER THAN 6 INCHES. CONSULT WITH MANUFACTURER FOR RE-DRILLING AND ANCHORING REQUIREMENTS OF FIELD-CUT PLATES.

15 FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN 1/8" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.



**PLAN VIEW  
RADIAL DETECTABLE  
WARNING FIELD ATTRIBUTES**

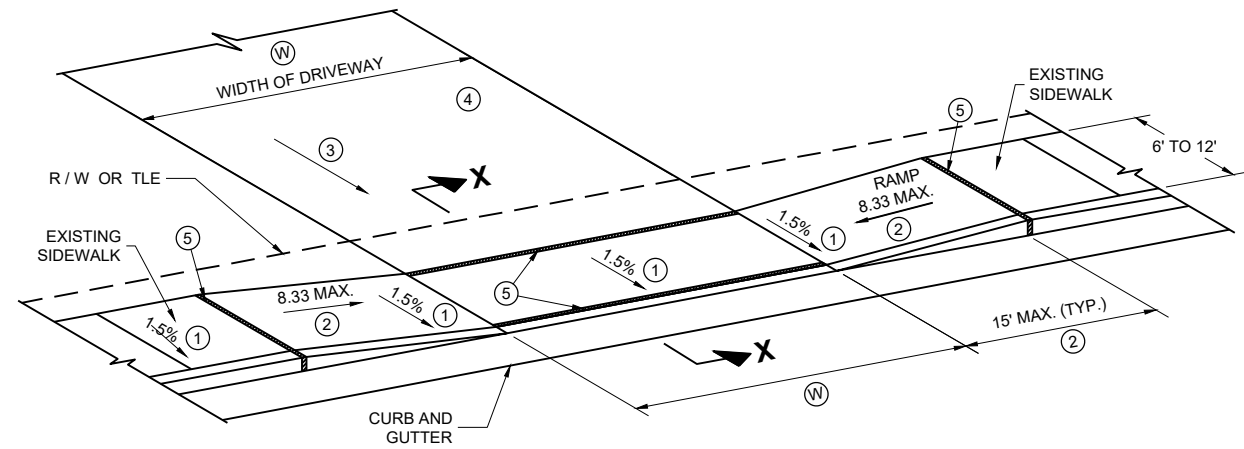


**PLAN VIEW  
RADIAL WEDGE PLATE  
CONNECTION DETAIL**

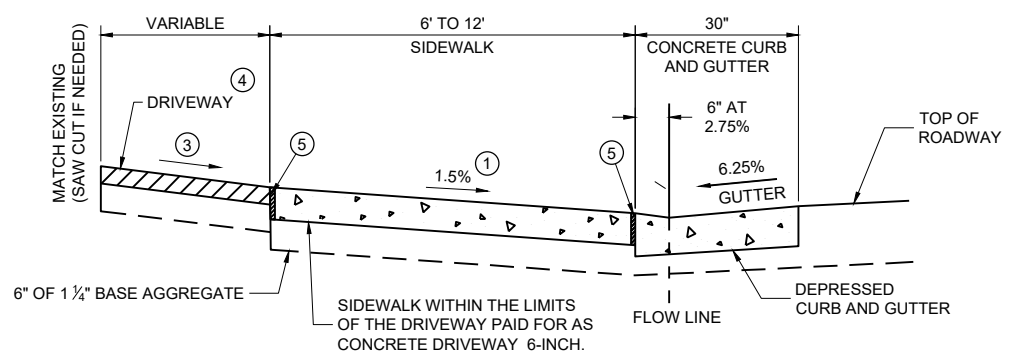
**CURB RAMPS  
RECTANGULAR AND RADIAL  
DETECTABLE WARNING PLATES**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

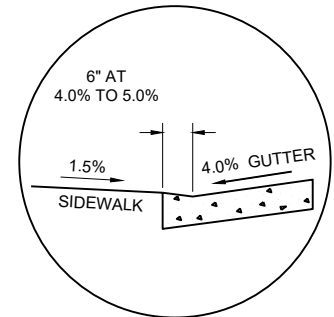
APPROVED  
July 2023 /S/ Rodney Taylor  
DATE ROADWAY STANDARDS DEVELOPMENT  
UNIT SUPERVISOR



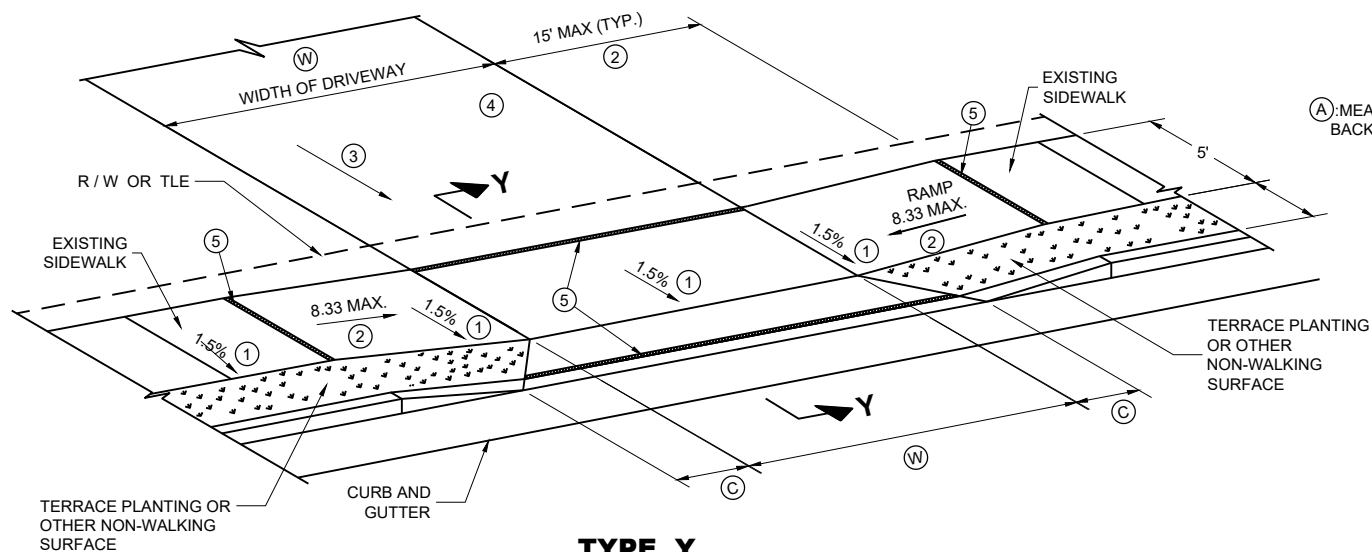
**TYPE X**  
**SIDEWALK ABUTS CURB AND GUTTER**  
**TERRACE VARIES 0 TO 3 FEET**



**SECTION X - X**

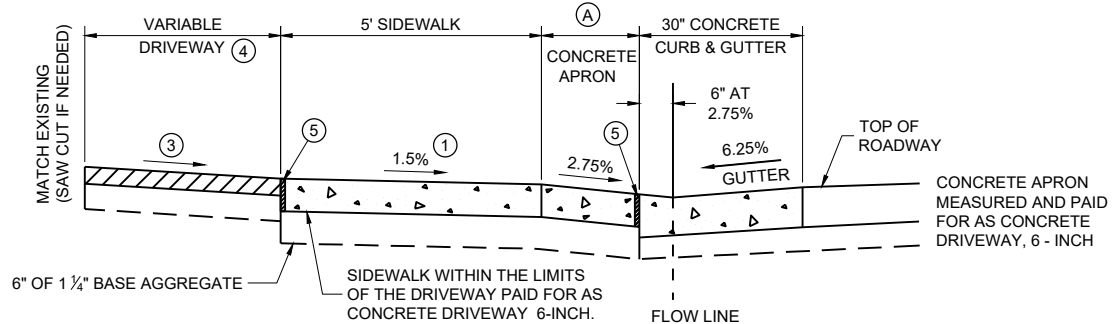


**SECTION X - X**  
**4% GUTTER SLOPE**



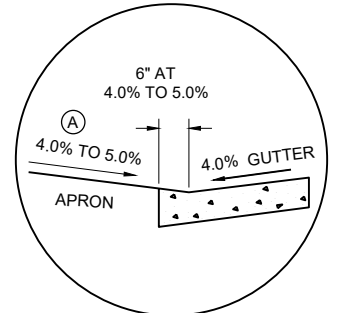
**TYPE Y**  
**SIDEWALK WITH NARROWER TERRACE**  
**TERRACE VARIES 4 TO 6 FEET**

(A): MEASURE FROM  
 BACK OF CURB



NOTE: SIDEWALK MAY BE DEPRESSED IN DRIVEWAY AREAS

**SECTION Y - Y**  
**DRIVEWAY DETAIL WITH CONCRETE**  
**CURB AND GUTTER**  
**(URBAN AND SUBURBAN)**



**SECTION Y - Y**  
**4% GUTTER SLOPE**

**TABLE Y**

(A) FEET	(C) FEET
3.5'	2.0'
4.5'	3.0'
5.5'	3.5'

(W): 12' MIN. - 24' MAX. RESIDENTIAL AND  
 NON-COMMERCIAL (PE & FE)  
 16' MIN. - 35' MAX. COMMERCIAL (CE)

**GENERAL NOTES**

PROVIDE CONSTRUCTION JOINTS ALONG THE CENTER OF THE CONCRETE FOR DRIVEWAYS UNDER 20 FEET IN WIDTH AND AT THE THIRD POINTS OVER 20 FEET IN WIDTH.

(W) IS SHOWN ON PLAN AND PROFILE SHEETS.

OFFSETS, ELEVATIONS, AND PERCENT GRADE ARE SHOWN ON THE CROSS SECTIONS.

① CONSTRUCTION TOLERANCE OF 0.5%± FOR SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.

② THE SIDEWALK RAMP MAXIMUM RUNNING SLOPE SHALL NOT REQUIRE THE RAMP LENGTH TO EXCEED 15 FEET TO AVOID CHASING THE SLOPE INDEFINITELY WHEN CONNECTING TO STEEP GRADES. WHEN APPLYING THE 15 FOOT MAXIMUM LENGTH, THE RUNNING SLOPE OF THE SIDEWALK SHALL BE AS FLAT AS FEASIBLE AND NOT EXCEED THE LONGITUDINAL GRADE OF THE ROADWAY. SLOPE SIDEWALK RAMP TOWARD APRON AS SHOWN WHERE THERE IS NO TERRACE OR WHERE THE TERRACE WIDTH IS LESS THAN 6 FEET WIDE.

③ **DRIVEWAY SLOPES: DESIRABLE MAXIMUM**  
 10.5% UP AWAY FROM SIDEWALK (SAG)  
 8.5% DOWN AWAY FROM SIDEWALK (CREST)  
 ABSOLUTE MAXIMUM 15% FOR BOTH CREST AND SAG

④ **DRIVEWAY TYPES**  
 · 6-INCH CONCRETE DRIVEWAY PAVEMENT OVER 6-INCH BASE AGGREGATE  
 · 2-INCH TO 3-INCH ASPHALTIC SURFACE OVER 6-INCH BASE AGGREGATE  
 · 6-INCH BASE AGGREGATE (MAY BE INCREASED FOR CLAY SUBGRADES.)

⑤ ½" EXPANSION JOINT FILLER

**DRIVEWAY AND**  
**SIDEWALK RAMPS**  
**TYPES X AND Y**

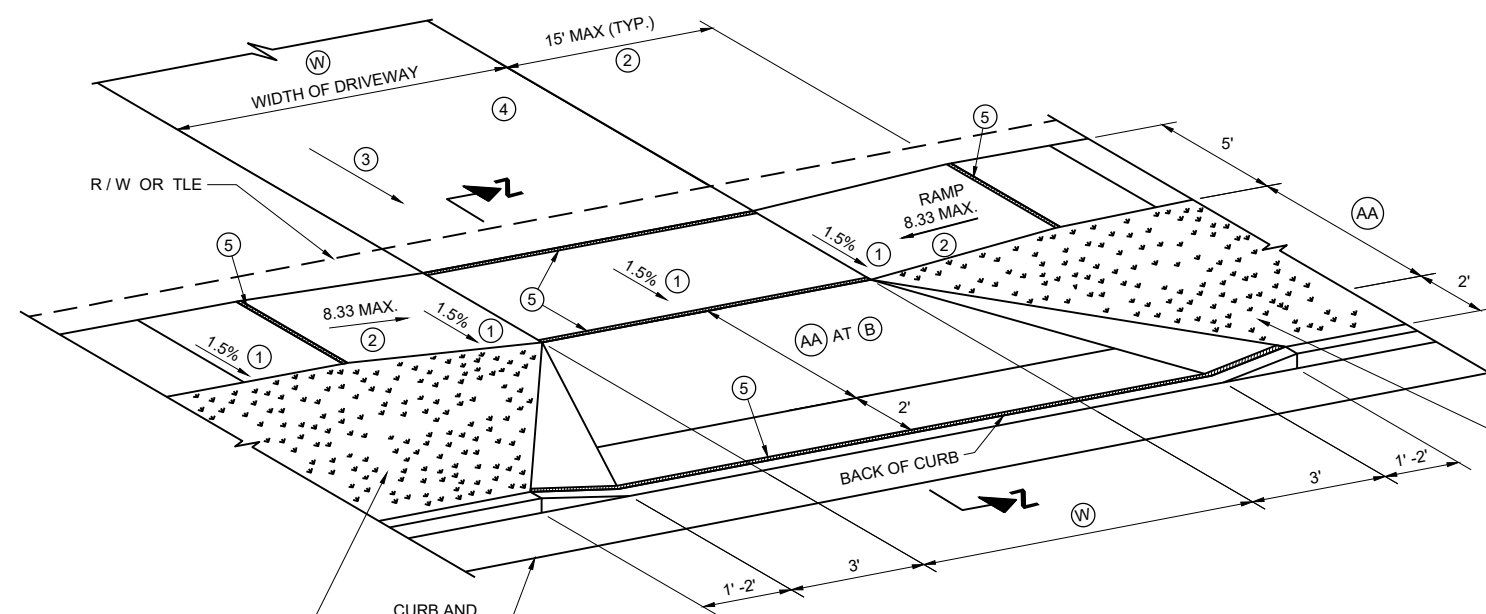
STATE OF WISCONSIN  
 DEPARTMENT OF TRANSPORTATION

APPROVED

July 2023  
 DATE

/s/ Rodney Taylor  
 ROADWAY STANDARDS DEVELOPMENT  
 UNIT SUPERVISOR

FHWA



**TYPE Z  
SIDEWALK WITH WIDER TERRACE  
TERRACE VARIES 7 TO 12 FEET**

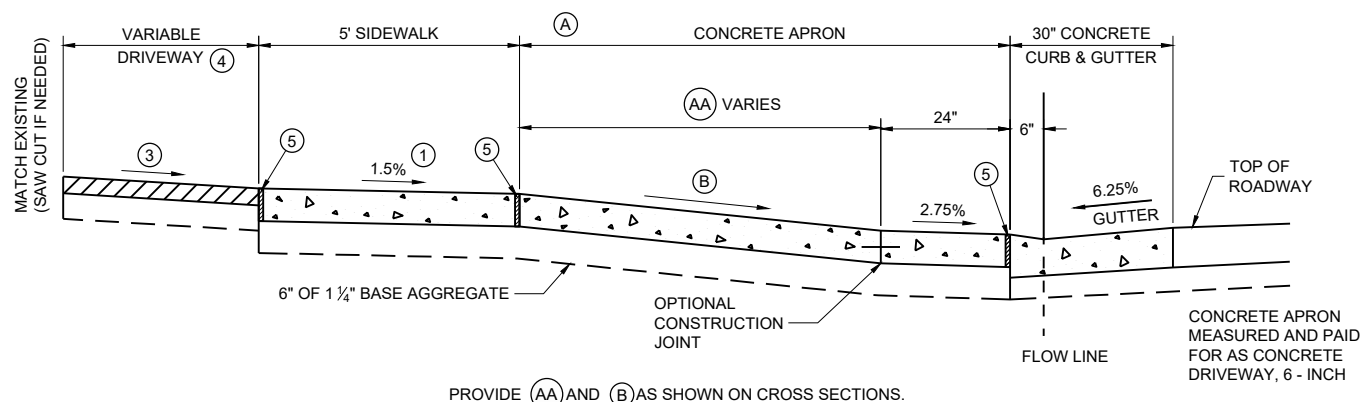
**GENERAL NOTES**

- PROVIDE CONSTRUCTION JOINTS ALONG THE CENTER OF THE CONCRETE FOR DRIVEWAYS UNDER 20 FEET IN WIDTH AND AT THE THIRD POINTS OVER 20 FEET IN WIDTH.
- (W) IS SHOWN ON PLAN AND PROFILE SHEETS.
- OFFSETS, ELEVATIONS, AND PERCENT GRADE ARE SHOWN ON THE CROSS SECTIONS.
- CONSTRUCTION TOLERANCE OF 0.5%± FOR SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.
  - THE SIDEWALK RAMP MAXIMUM RUNNING SLOPE SHALL NOT REQUIRE THE RAMP LENGTH TO EXCEED 15 FEET TO AVOID CHASING THE SLOPE INDEFINITELY WHEN CONNECTING TO STEEP GRADES. WHEN APPLYING THE 15 FOOT MAXIMUM LENGTH, THE RUNNING SLOPE OF THE SIDEWALK SHALL BE AS FLAT AS FEASIBLE AND NOT EXCEED THE LONGITUDINAL GRADE OF THE ROADWAY.
  - DRIVEWAY SLOPES: DESIRABLE MAXIMUM**  
10.5% UP AWAY FROM SIDEWALK (SAG)  
8.5% DOWN AWAY FROM SIDEWALK (CREST)  
ABSOLUTE MAXIMUM 15% FOR BOTH CREST AND SAG
  - DRIVEWAY TYPES**  
- 6-INCH CONCRETE DRIVEWAY PAVEMENT OVER 6-INCH BASE AGGREGATE  
- 2-INCH TO 3-INCH ASPHALTIC SURFACE OVER 6-INCH BASE AGGREGATE  
- 6-INCH BASE AGGREGATE (MAY BE INCREASED FOR CLAY SUBGRADES.)
  - ½" EXPANSION JOINT FILLER.

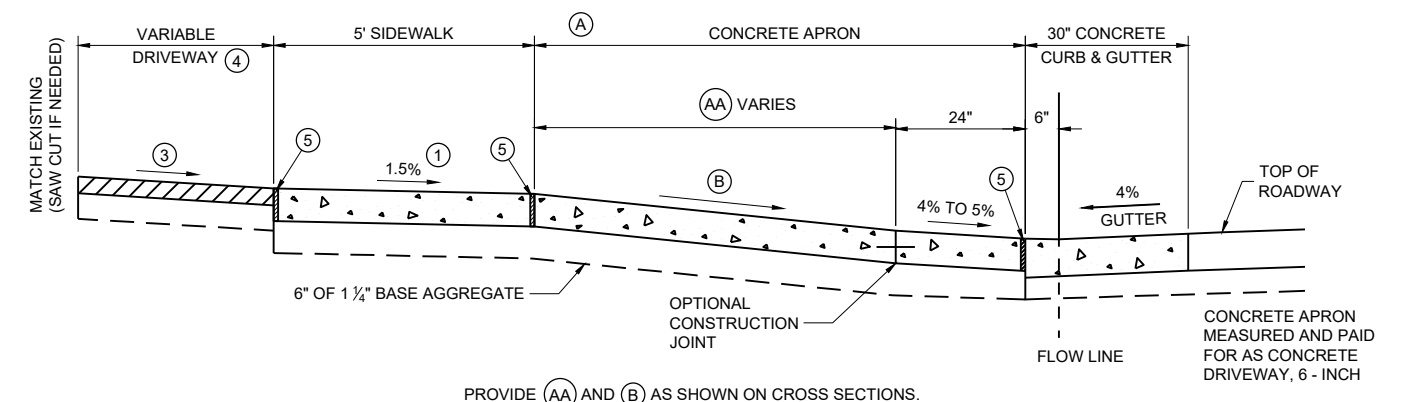
**TABLE Z**

(AA) FEET	(B) % 6.25% GUTTER	(B) % 4% GUTTER
4.5'	11.5%	9% TO 11.5%
5.5'	9% TO 11.5%	8% TO 11.5%
6.5'	8% TO 11.5%	6% TO 11.5%
7.5'	7% TO 11.5%	6% TO 11.5%
8.5'	6% TO 11.5%	5% TO 11.5%
9.5'	5% TO 11.5%	4% TO 11.5%

(W): 12' MIN. - 24' MAX. RESIDENTIAL AND NON-COMMERCIAL (PE & FE)  
16' MIN. - 35' MAX. COMMERCIAL (CE)



**6.25% GUTTER SLOPE**



**4% GUTTER SLOPE**

NOTE: SIDEWALK MAY BE DEPRESSED IN DRIVEWAY AREAS FOR (B) VALUES NOT SHOWN IN TABLE Z.  
SIDEWALK WITHIN THE LIMITS OF THE DRIVEWAY PAID FOR AS CONCRETE DRIVEWAY 6-INCH.

**SECTION Z - Z  
DRIVEWAY DETAIL WITH CONCRETE CURB AND GUTTER  
(URBAN AND SUBURBAN)**

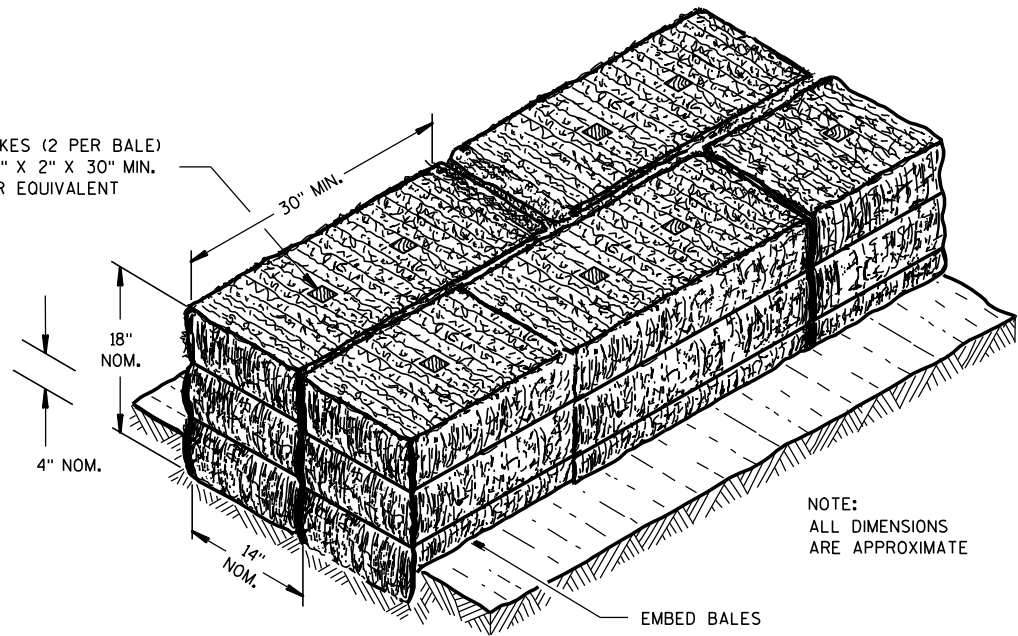
**DRIVEWAY AND SIDEWALK RAMPS  
TYPE Z**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
July 2023 /S/ Rodney Taylor  
DATE ROADWAY STANDARDS DEVELOPMENT  
FHWA UNIT SUPERVISOR



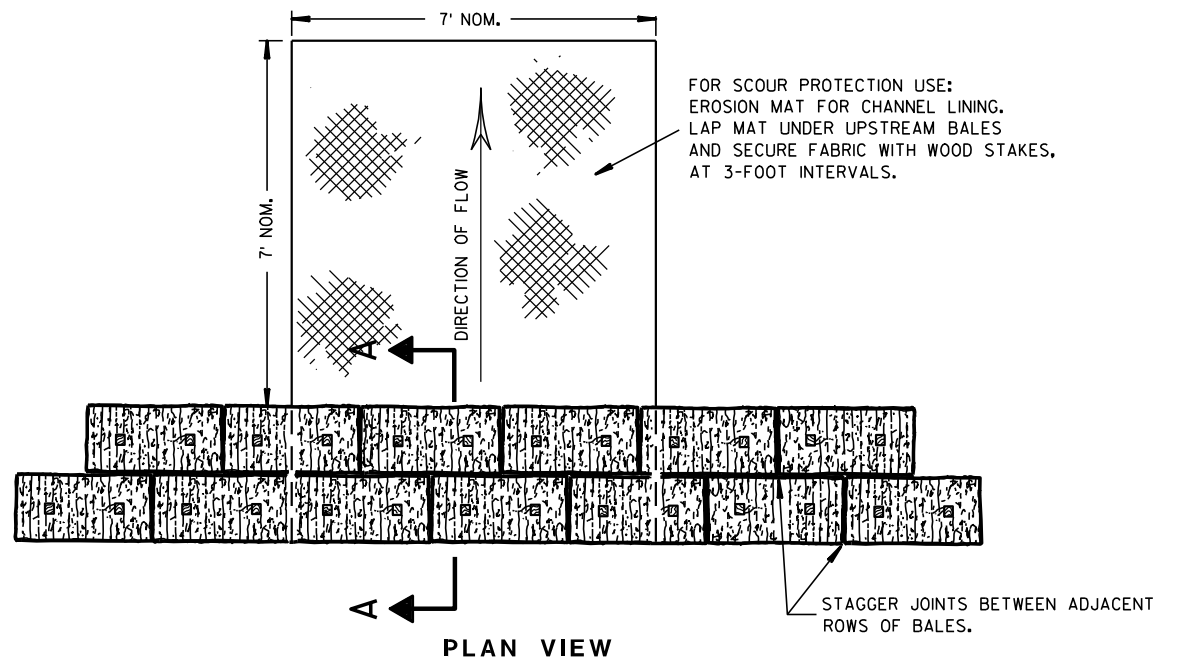
WOOD STAKES (2 PER BALE)  
NOMINAL 2" X 2" X 30" MIN.  
LENGTH OR EQUIVALENT



NOTE:  
ALL DIMENSIONS  
ARE APPROXIMATE

EMBED BALES

SECTION A-A

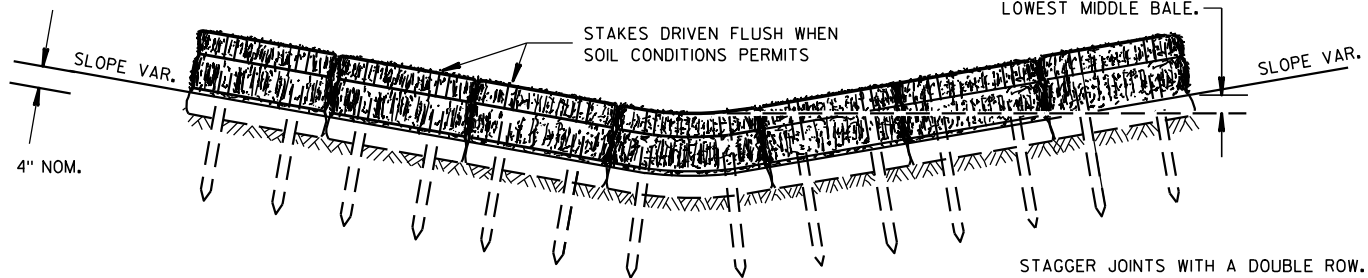


FOR SCOUR PROTECTION USE:  
EROSION MAT FOR CHANNEL LINING.  
LAP MAT UNDER UPSTREAM BALES  
AND SECURE FABRIC WITH WOOD STAKES,  
AT 3-FOOT INTERVALS.

STAGGER JOINTS BETWEEN ADJACENT  
ROWS OF BALES.

PLAN VIEW

BOTTOM ELEVATION OF END BALE SHALL  
BE EQUAL TO OR GREATER THAN TOP OF  
LOWEST MIDDLE BALE.



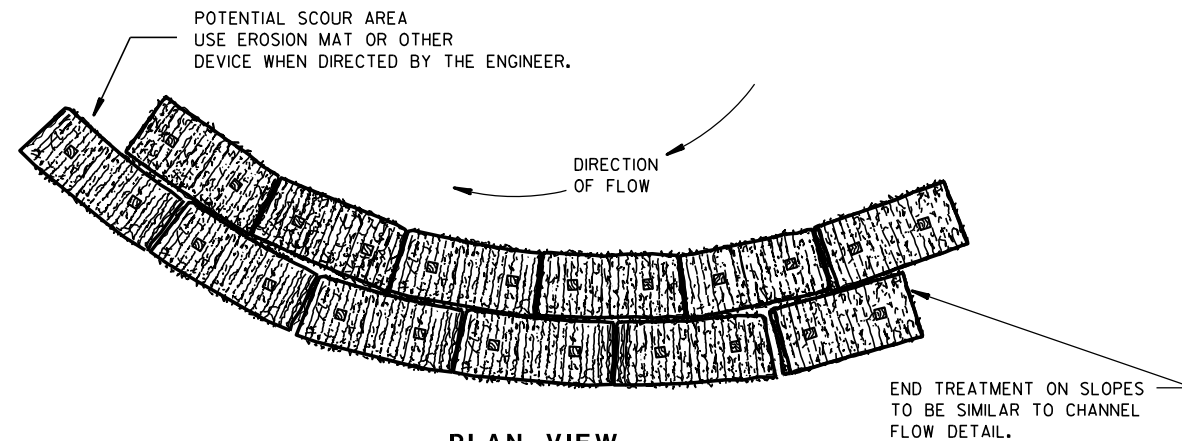
FRONT ELEVATION

TEMPORARY DITCH CHECK USING EROSION BALES ①

GENERAL NOTES

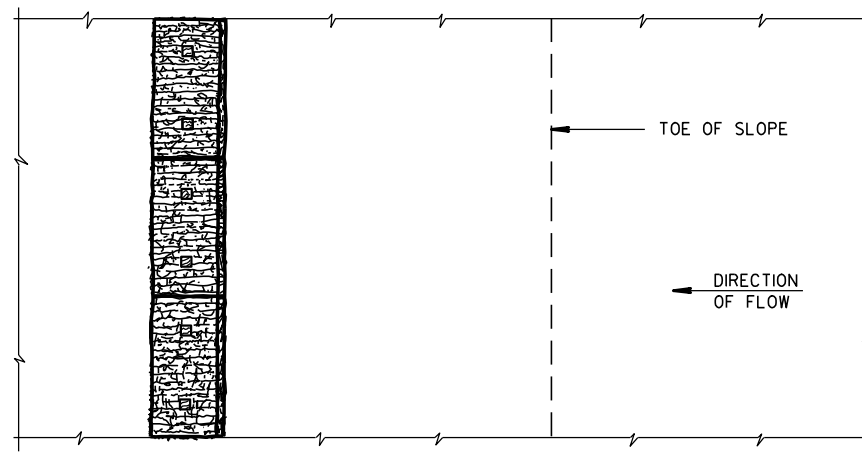
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

- ① TEMPORARY DITCH CHECKS EITHER EROSION BALES OR MANUFACTURED SHALL BE PAID FOR UNDER THE BID ITEM OF TEMPORARY DITCH CHECK. THE DEPARTMENT WILL NOT PAY FOR TEMPORARY DITCH CHECKS CONSTRUCTED OF A SINGLE ROW OF EROSION BALES.

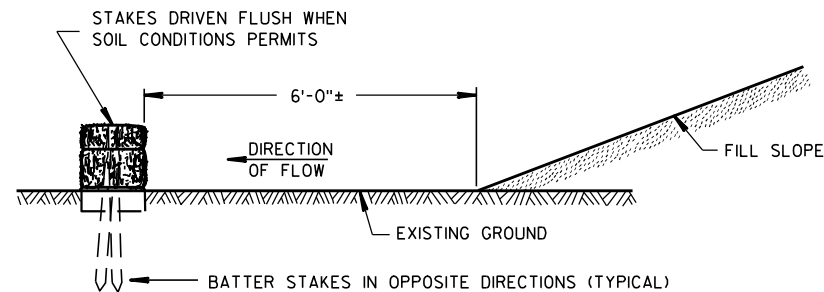


PLAN VIEW

WHEN ALTERING THE DIRECTION OF FLOW



PLAN VIEW



FRONT ELEVATION

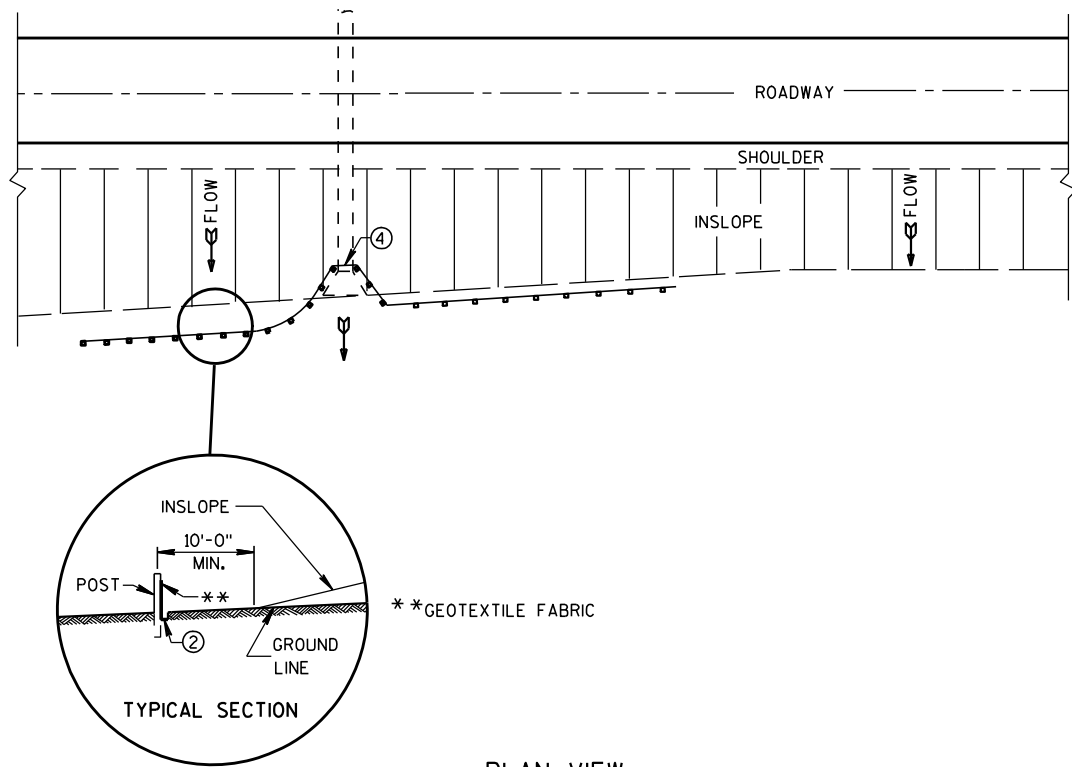
WHEN EXISTING GROUND SLOPES AWAY FROM FILL SLOPE

EROSION BALES FOR SHEET FLOW

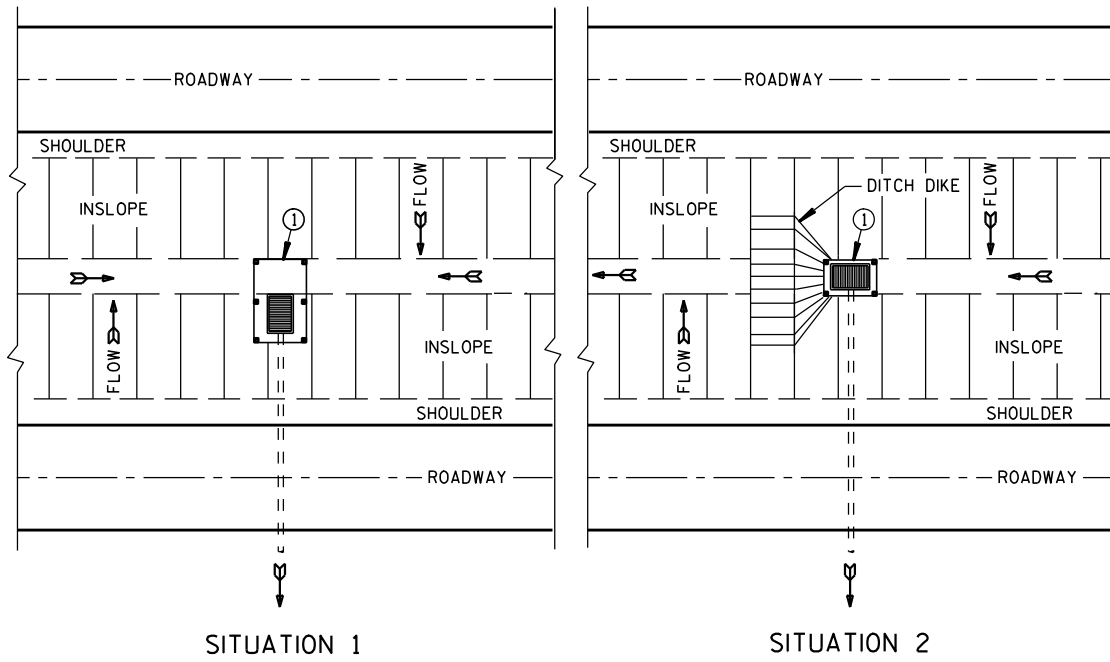
TYPICAL INSTALLATIONS OF  
EROSION BALES / TEMPORARY  
DITCH CHECKS

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
 6/04/02 /S/ Beth Canestra  
 DATE CHIEF ROADWAY DEVELOPMENT ENGINEER  
 FHWA



PLAN VIEW  
TYPICAL APPLICATION OF SILT FENCE

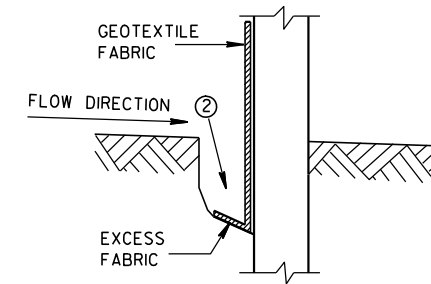


SITUATION 1      SITUATION 2  
PLAN VIEW  
SILT FENCE AT MEDIAN SURFACE DRAINS

**GENERAL NOTES**

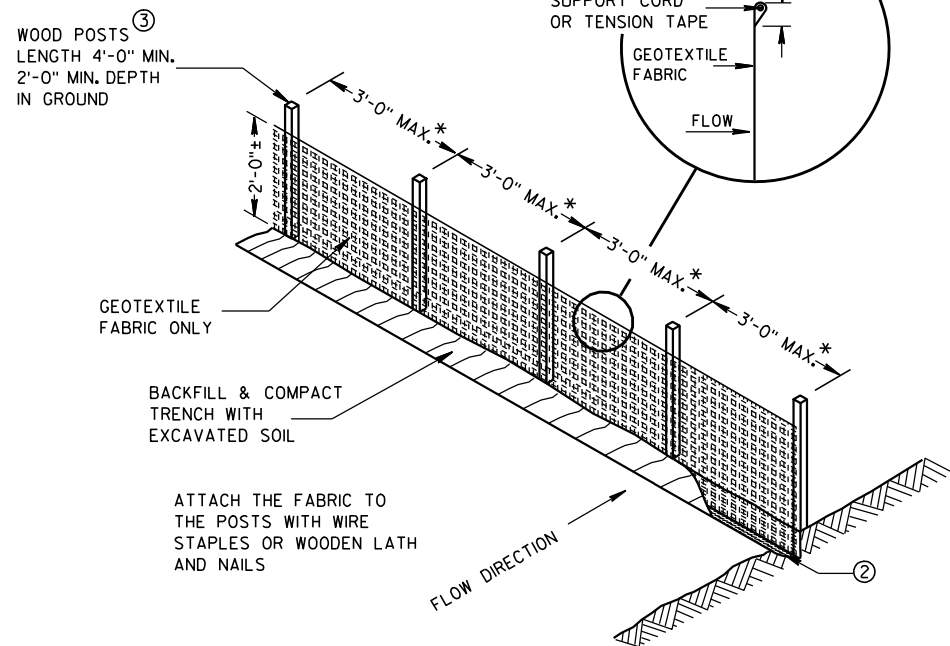
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

- ① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- ③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1 1/8" X 1 1/8" OF OAK OR HICKORY.
- ④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- ⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



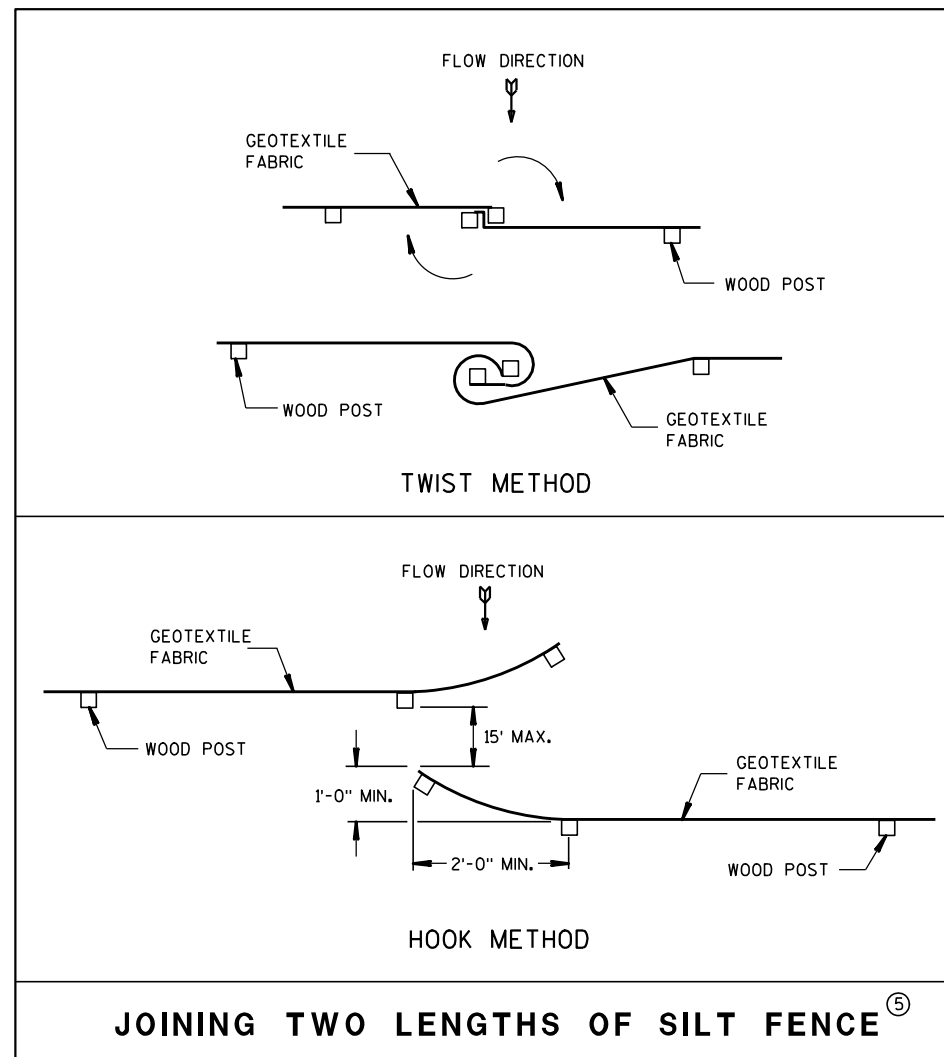
TRENCH DETAIL

NOTE: ADDITIONAL POST DEPTH OR TIE BACKS MAY BE REQUIRED IN UNSTABLE SOILS

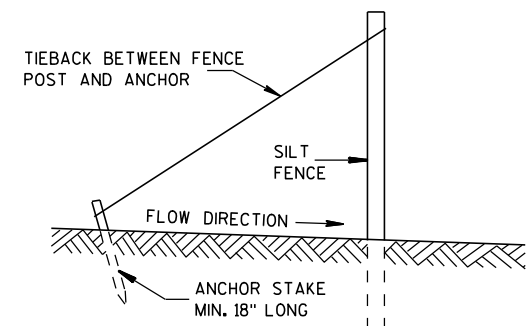


SILT FENCE

\* NOTE: 8'-0" POST SPACING ALLOWED IF A WOVEN GEOTEXTILE FABRIC IS USED.



JOINING TWO LENGTHS OF SILT FENCE ⑤



SILT FENCE TIE BACK  
(WHEN REQUIRED BY THE ENGINEER)

**SILT FENCE**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

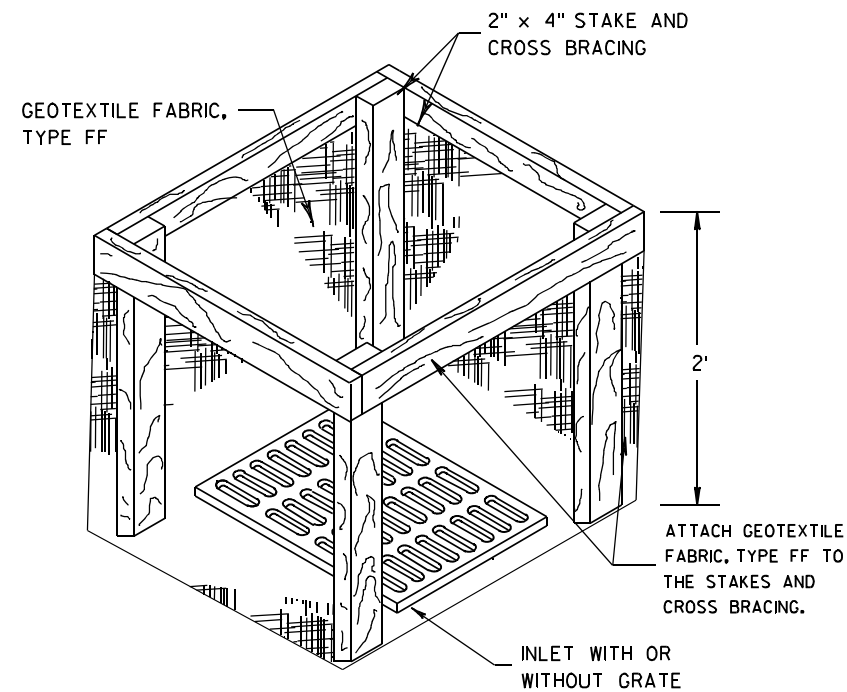
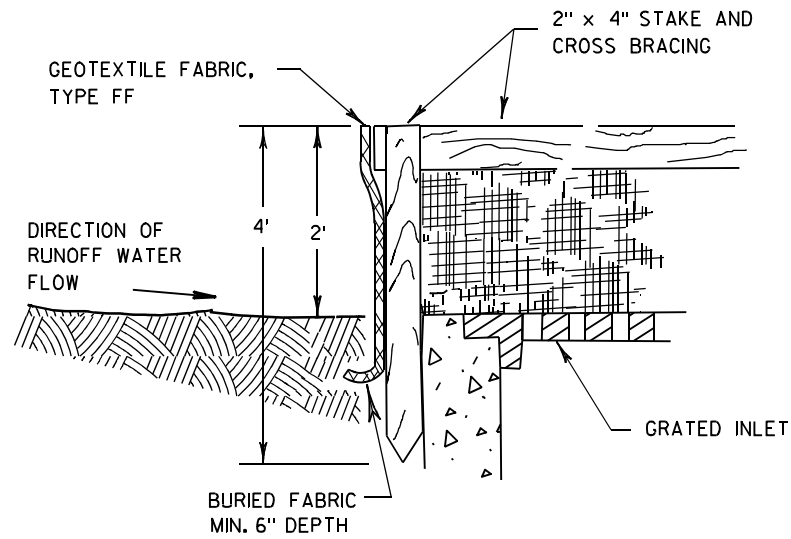
APPROVED

4-29-05

DATE

FHWA

/S/ Beth Cannestra  
CHIEF ROADWAY DEVELOPMENT ENGINEER



**INLET PROTECTION, TYPE A**

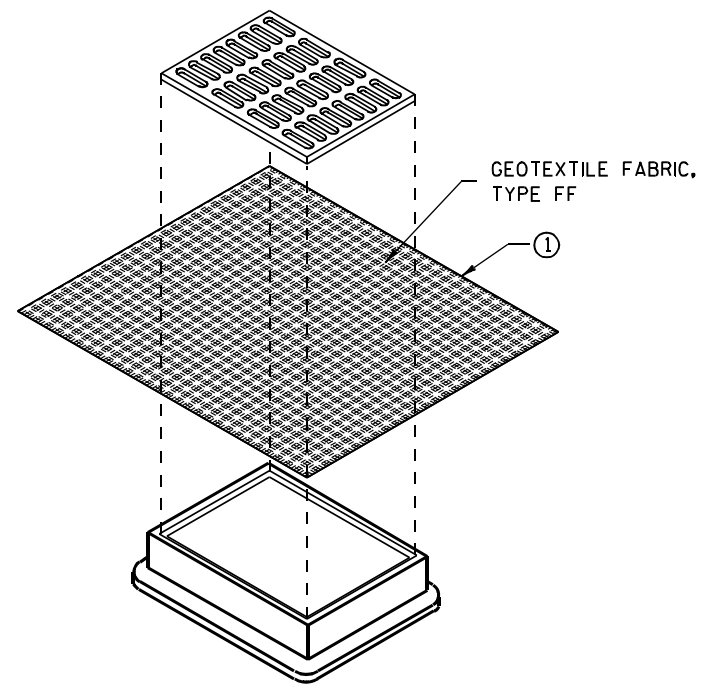
**GENERAL NOTES**

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

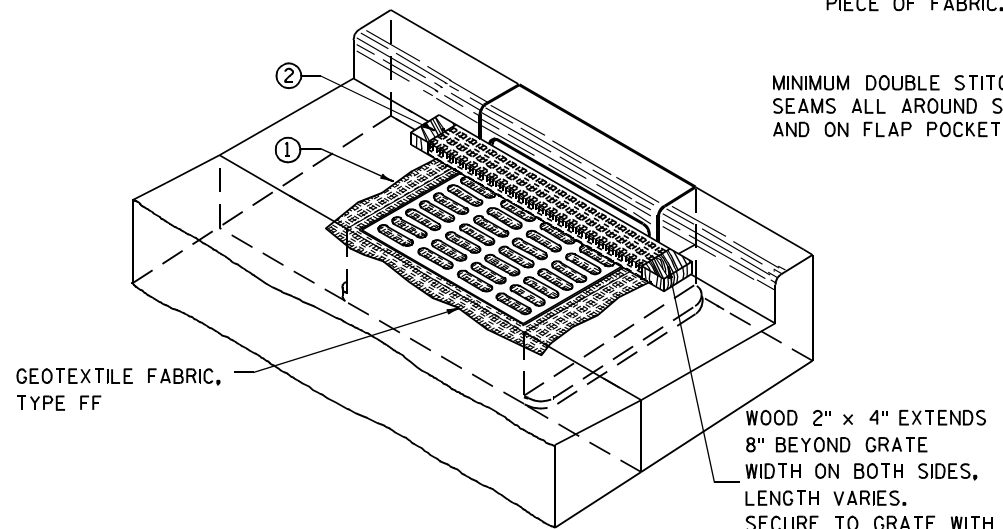
WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- ① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- ② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- ③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



**INLET PROTECTION, TYPE B  
(WITHOUT CURB BOX)**

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



**INLET PROTECTION, TYPE C (WITH CURB BOX)**

**INSTALLATION NOTES**

**TYPE B & C**

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

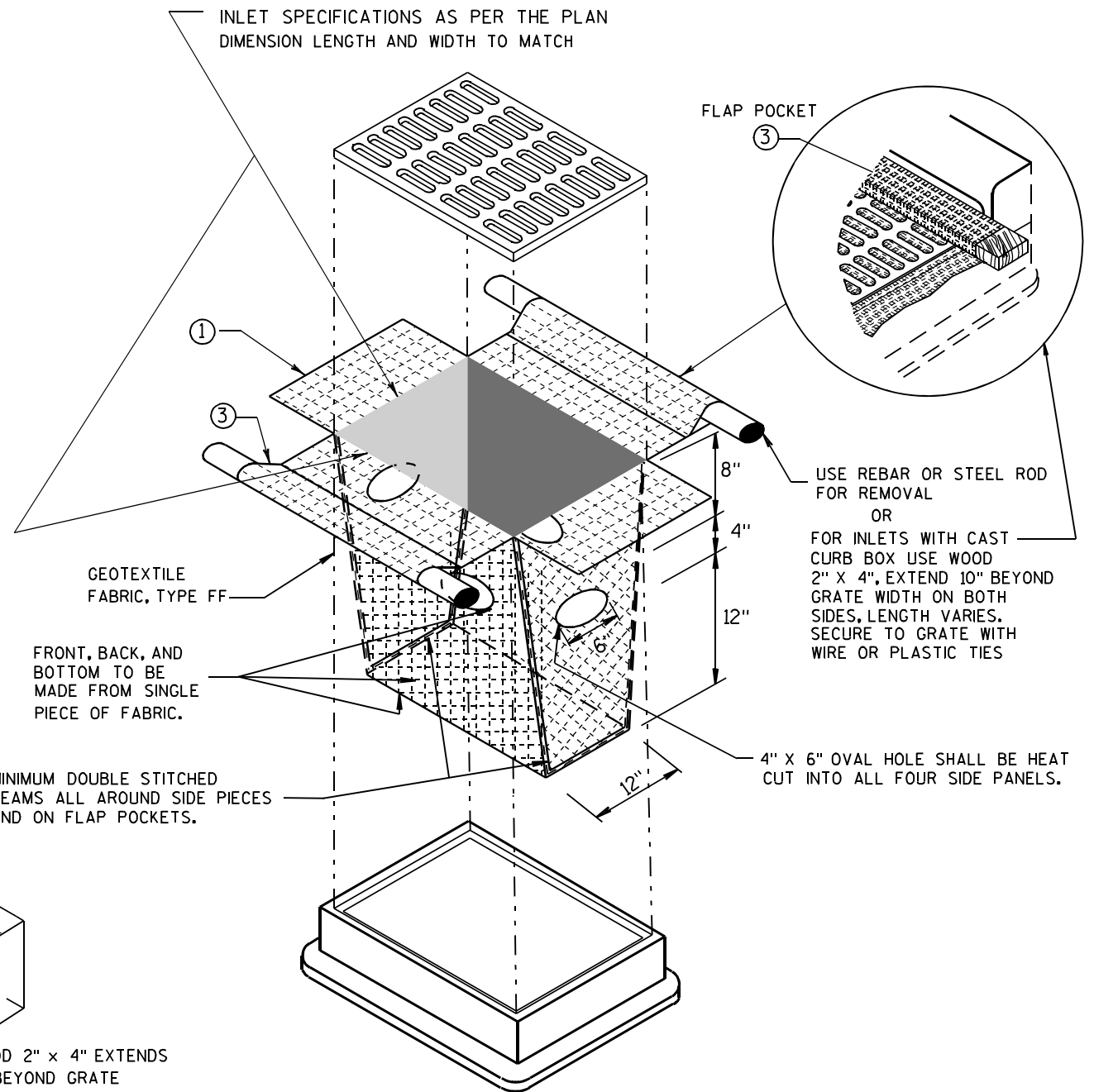
THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

**TYPE D**

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.



**INLET PROTECTION, TYPE D**

(CAN BE INSTALLED IN ANY INLET TYPE WITH OR WITHOUT A CURB BOX AS PER NOTE ②)

<b>INLET PROTECTION TYPE A, B, C, AND D</b>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 10/16/02 DATE	/s/ Beth Connestra CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA	

**GENERAL NOTES**

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

TRACKING PAD SHALL BE INSPECTED DAILY. DEFICIENT AREAS SHALL BE REPAIRED OR REPLACED IMMEDIATELY.

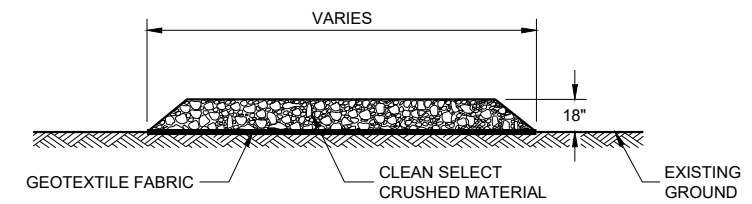
TRACKING PAD TO BE REMOVED AFTER CONSTRUCTION IS COMPLETED.

TRACKING PAD SHALL BE THE FULL WIDTH OF THE EGRESS POINT.

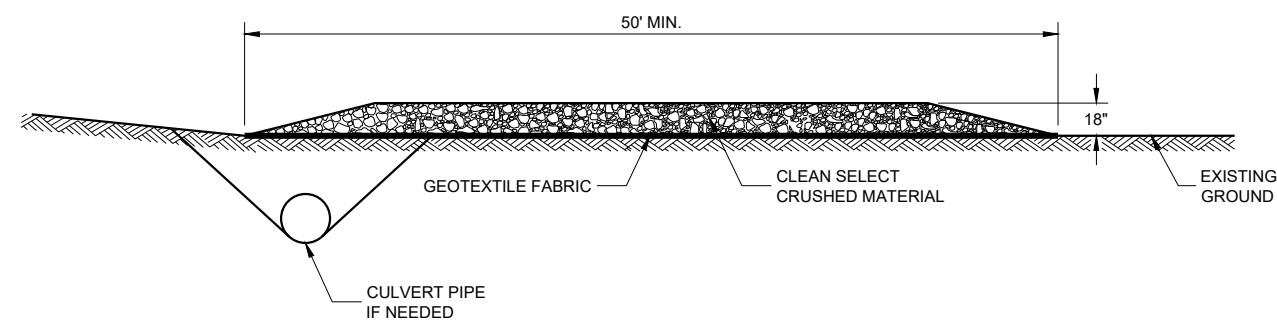
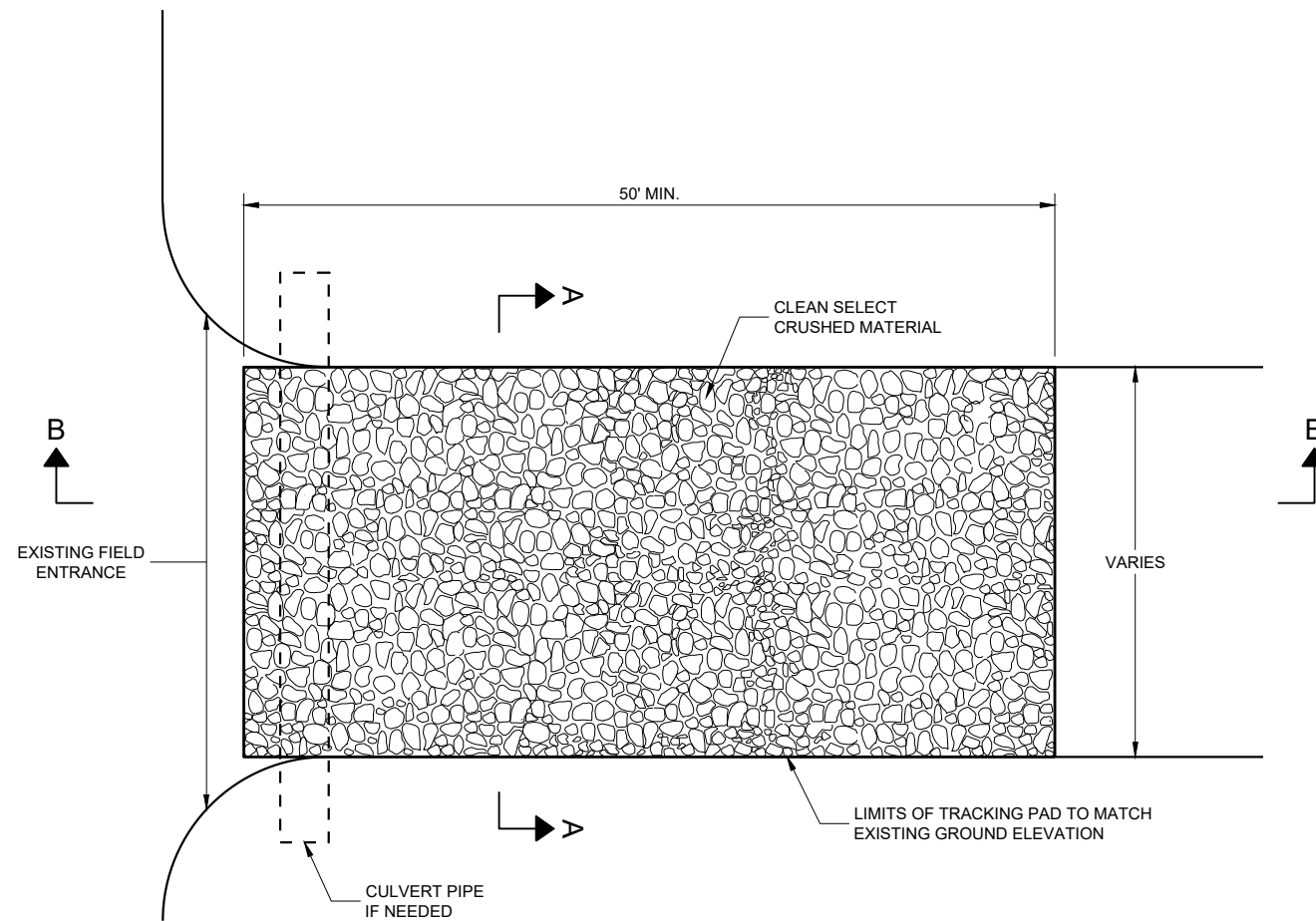
SURFACE WATER MUST BE PREVENTED FROM PASSING THROUGH THE TRACKING PAD. FLOWS SHALL BE DIVERTED AWAY, AROUND OR CONVEYED UNDER THE TRACKING PAD.

CULVERT PIPE OR OTHER BMP USED TO DIVERT WATER AWAY, AROUND OR UNDER THE TRACKING PAD SHALL BE DESIGNED TO CONVEY THE 2 YEAR - 24 HOUR EVENT.

THE COST OF ADDITIONAL BMP TO DIVERT WATER ARE INCIDENTAL TO THE TRACKING PAD BID ITEM.



**SECTION A - A**



**SECTION B - B**

**TRACKING PAD**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
3/24/2011 DATE /S/ Jerry H. Zogg  
ROADWAY STANDARDS DEVELOPMENT ENGINEER

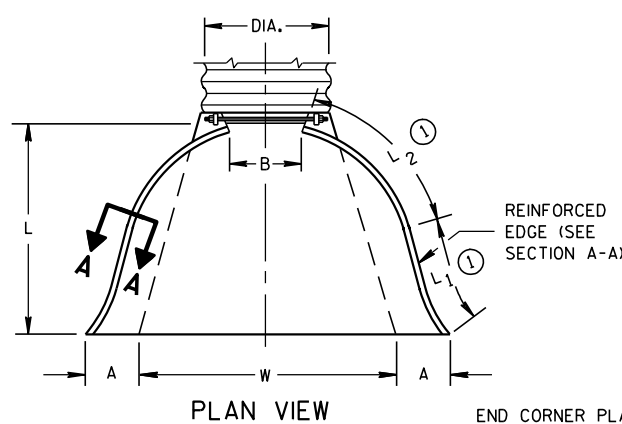
FHWA

METAL APRON ENDWALLS											
PIPE DIA. (IN.)	MIN. THICK. (Inches)		DIMENSIONS (Inches)							APPROX. SLOPE	BODY
	STEEL	ALUM.	A (±1")	B (MAX.)	H (±1")	L (±1 1/2")	L1	L2	W (±2")		
12	.064	.060	6	6	6	21	12	17 1/2	24	2 1/2 to 1	1 Pc.
15	.064	.060	7	8	6	26	14	21 3/4	30	2 1/2 to 1	1 Pc.
18	.064	.060	8	10	6	31	15	28 1/4	36	2 1/2 to 1	1 Pc.
21	.064	.060	9	12	6	36	18	29 5/8	42	2 1/2 to 1	1 Pc.
24	.064	.075	10	13	6	41	18	37 1/4	48	2 1/2 to 1	1 Pc.
30	.079	.075	12	16	8	51	18	52 1/4	60	2 1/2 to 1	1 Pc.
36	.079	.105	14	19	9	60	24	59 3/4	72	2 1/2 to 1	2 Pc.
42	.109	.105	16	22	11	69	24	75 5/8	84	2 1/2 to 1	2 Pc.
48	.109	.105	18	27	12	78	24	81	90	2 1/4 to 1	3 Pc.
54	.109	.105	18	30	12	84	30	85 1/2	102	2 1/4 to 1	3 Pc.
60	.109x	.105x	18	33	12	87	—	—	114	2 to 1	3 Pc.
66	.109x	.105x	18	36	12	87	—	—	120	2 to 1	3 Pc.
72	.109x	.105x	18	39	12	87	—	—	126	2 to 1	3 Pc.
78	.109x	.105x	18	42	12	87	—	—	132	1 1/2 to 1	3 Pc.
84	.109x	.105x	18	45	12	87	—	—	138	1 1/2 to 1	3 Pc.
90	.109x	.105x	18	37	12	87	—	—	144	1 1/2 to 1	3 Pc.
96	.109x	.105x	18	35	12	87	—	—	150	1 1/2 to 1	3 Pc.

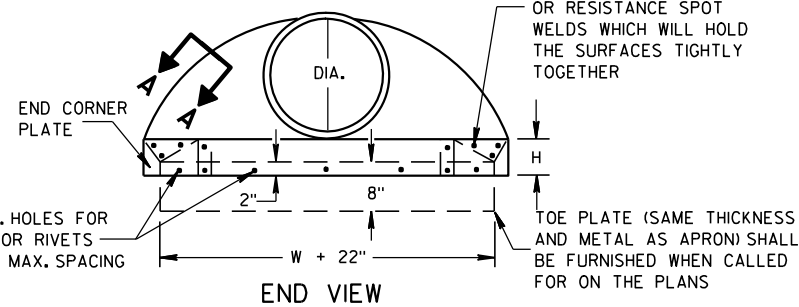
\* EXCEPT CENTER PANEL SEE GENERAL NOTES

REINFORCED CONCRETE APRON ENDWALLS									
PIPE DIA. (IN.)	DIMENSIONS (Inches)							APPROX. SLOPE	
	T	A	B	C	D	E	G		
12	2	4	24	48 1/8	72 1/8	24	2	3 to 1	
15	2 1/4	6	27	46	73	30	2 1/4	3 to 1	
18	2 1/2	9	27	46	73	36	2 1/2	3 to 1	
21	2 3/4	9	36	37 1/2	73 1/2	42	2 3/4	3 to 1	
24	3	9 1/2	43 1/2	30	73 1/2	48	3	3 to 1	
27	3 1/4	10 1/2	49 1/2	24	73 1/2	54	3 1/4	3 to 1	
30	3 1/2	12	54	19 3/4	73 1/2	60	3 1/2	3 to 1	
36	4	15	63	34 3/4	97 3/4	72	4	3 to 1	
42	4 1/2	21	63	35	98	78	4 1/2	3 to 1	
48	5	24	72	26	98	84	5	3 to 1	
54	5 1/2	27	65	33 1/4-35	98 1/4-100	90	5 1/2	2 1/2 to 1	
60	6	30-35	60	39	99	96	5	2 to 1	
66	6 1/2	24-30	72-78	21-27	99	102	5 1/2	2 to 1	
72	7	24-36	78	21	99	108	6	2 to 1	
78	7 1/2	24-36	78	21	99	114	6 1/2	2 to 1	
84	8	36	90 1/2	21	111 1/2	120	6 1/2	1 1/2 to 1	
90	8 1/2	41	87 1/2	24	111 1/2	132	6 1/2	1 1/2 to 1	

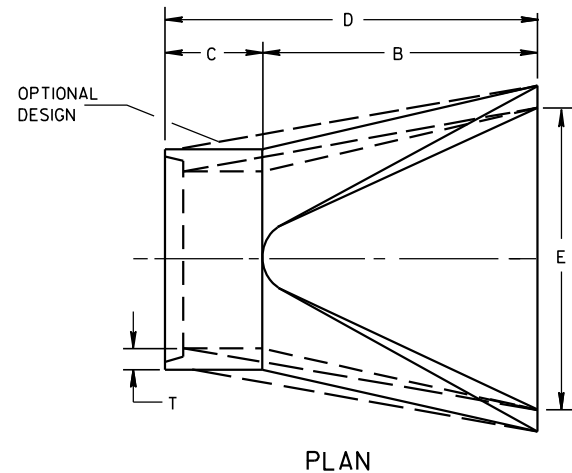
\* MINIMUM  
\*\* MAXIMUM



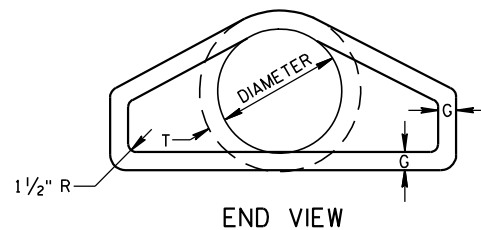
END CORNER PLATES MAY BE FASTENED TO APRON PROPER BY BOLTS, RIVETS, OR RESISTANCE SPOT WELDS WHICH WILL HOLD THE SURFACES TIGHTLY TOGETHER



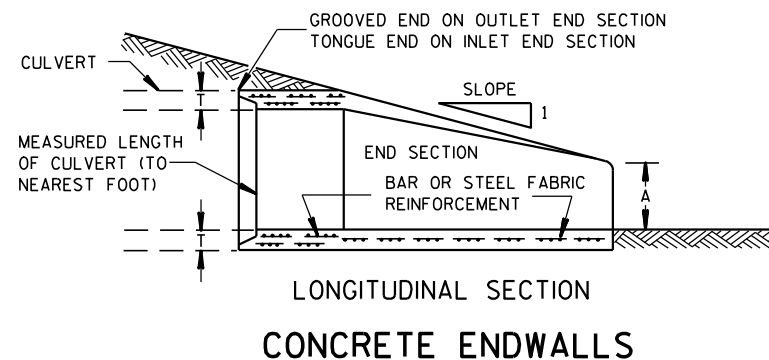
SIDE ELEVATION  
METAL ENDWALLS



PLAN

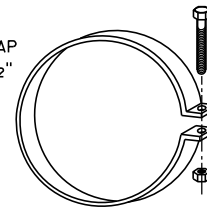


END VIEW

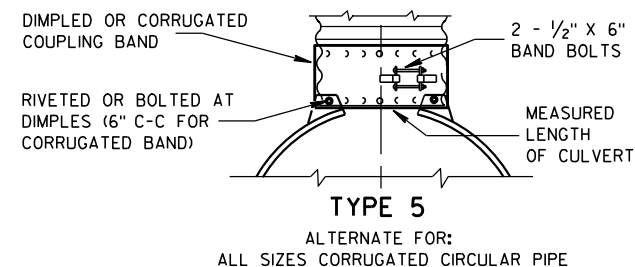
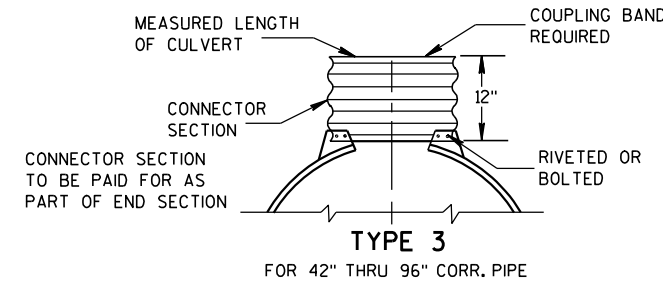
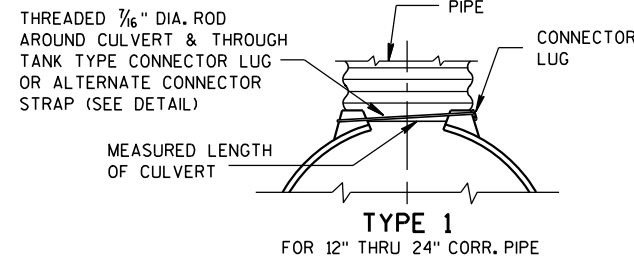


LONGITUDINAL SECTION  
CONCRETE ENDWALLS

1" WIDE, 12 GA. (0.109" THICK) GALVANIZED STRAP WITH STANDARD 6" X 1/2" BAND BOLT AND NUT



ALTERNATE FOR TYPE 1 CONNECTION  
END SECTION CONNECTOR STRAP



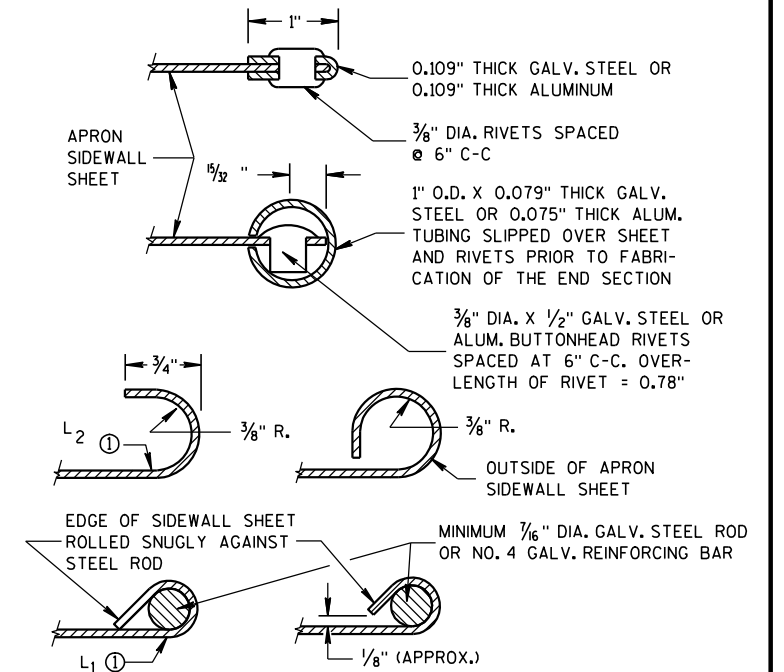
NOTE: DIMPLED BAND FITS OVER OUTSIDE OF ENDWALL, AND CORRUGATED BAND FITS INSIDE ENDWALL. DIMPLED BAND MAY BE USED WITH HELICALLY CORRUGATED PIPE.

FOR CIRCUMFERENTIALLY CORRUGATED PIPE USE ENDWALL CONNECTION DETAILS 1, 2, 3 OR 5 AS APPLICABLE.

FOR HELICALLY CORRUGATED PIPE USE ENDWALL CONNECTION DETAILS 1, 2 OR 5.

FOR HELICALLY CORRUGATED PIPES WITH TWO CIRCUMFERENTIAL CORRUGATIONS AT EACH END USE ENDWALL CONNECTION DETAILS 1, 2 OR 3.

CONNECTION DETAILS



SECTION A-A

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

CONCRETE CULVERT ENDWALLS MAY NOT BE USED WITH GALVANIZED STEEL OR ALUMINUM CULVERT PIPE OR VICE VERSA. GALVANIZED STEEL OR ALUMINUM ENDWALLS SHALL NORMALLY BE INSTALLED ON CULVERT PIPE OF THE SAME METAL.

ALL THREE PIECE STEEL APRON ENDWALLS FOR 60" DIAMETER PIPE AND LARGER SHALL HAVE 0.109" SIDES AND 0.138" CENTER PANELS. ALL THREE PIECE ALUMINUM APRON ENDWALLS FOR 60" DIAMETER PIPE AND LARGER SHALL HAVE 0.105" SIDES AND 0.134" CENTER PANELS. THE WIDTH OF CENTER PANELS SHALL BE GREATER THAN 20 PERCENT OF THE PIPE PERIMETER.

LAP SEAMS SHALL BE TIGHTLY JOINED BY GALVANIZED RIVETS OR BOLTS FOR STEEL UNITS AND ALUMINUM RIVETS AND BOLTS FOR ALUMINUM UNITS. FOR THE 60" THROUGH 96" DIAMETER APRON ENDWALL SIZES, THE REINFORCED EDGES AND CENTER PANEL SEAMS SHALL BE FURTHER REINFORCED WITH GALVANIZED STEEL OR ALUMINUM STIFFENER ANGLES. THE ANGLES SHALL BE ATTACHED BY GALVANIZED NUTS AND BOLTS FOR STEEL UNITS AND ALUMINUM NUTS AND BOLTS FOR ALUMINUM UNITS.

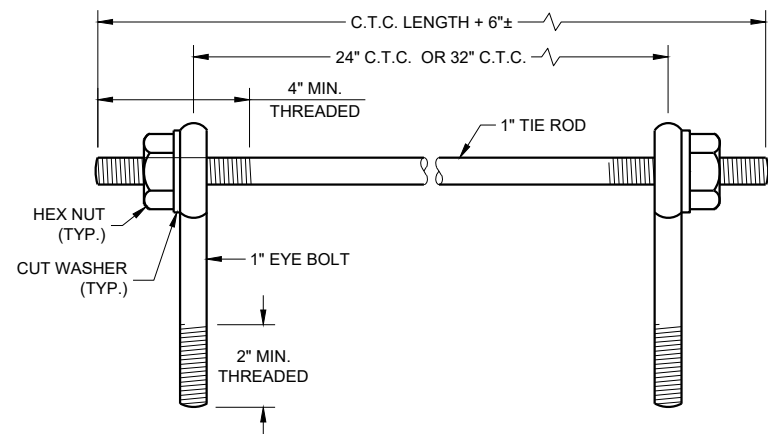
WHERE TWO OR MORE PIPES WITH APRON ENDWALLS ARE LAID ADJACENT TO EACH OTHER, THEY SHALL BE SEPARATED BY A DISTANCE SUFFICIENT TO PROVIDE A MINIMUM CLEARANCE OF 6 INCHES BETWEEN APRON ENDWALLS.

① FOR PIPE SIZES UP TO 60" DIAMETER, A 180° ROLLED EDGE MAY BE USED INSTEAD OF STEEL ROD REINFORCEMENT. SEE SECTION A-A.

APRON ENDWALLS FOR  
CULVERT PIPE

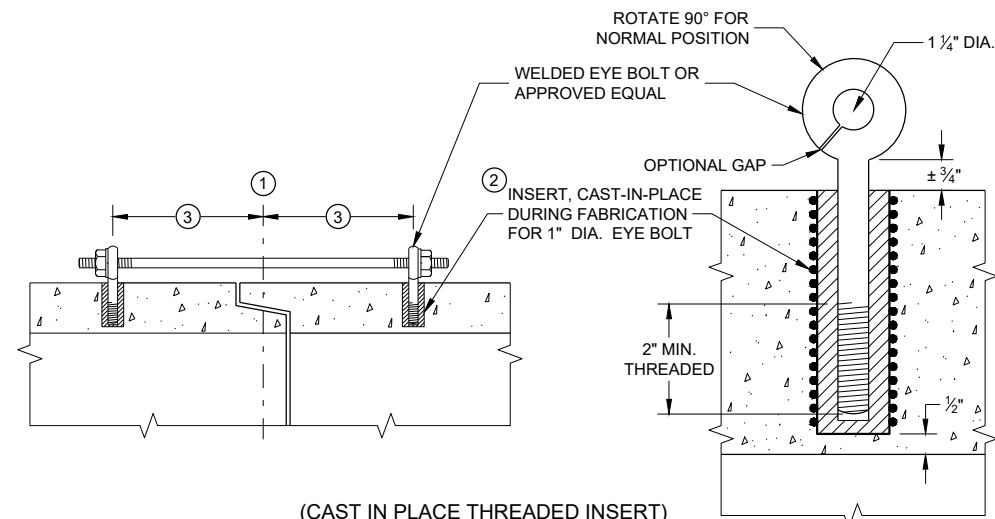
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
11/30/94 /S/ Rory L. Rhinesmith  
DATE CHIEF ROADWAY DEVELOPMENT ENGINEER  
FHWA



**EYE BOLTS AND TIE ROD**

**EYE BOLT AND TIE ROD ASSEMBLY (ALTERNATE NO. 1)**



(CAST IN PLACE THREADED INSERT)  
**LONGITUDINAL SECTIONS**

**GENERAL NOTES**

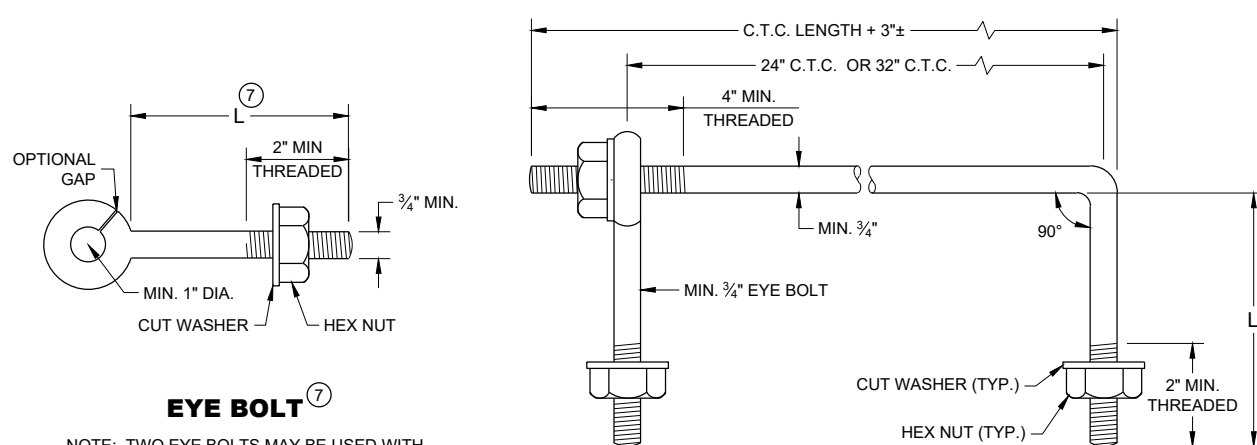
DETAILS OF CONSTRUCTION, MATERIALS, AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

CONCRETE CULVERT AND STORM SEWER PIPE SHALL BE TIED TOGETHER IN THE MANNER ILLUSTRATED BY THIS DETAIL AT LOCATIONS DESIGNATED IN THE STANDARD SPECIFICATIONS AND THE PLAN. THE CONTRACTOR MAY USE EITHER ALTERNATE 1, 2 OR 3 FOR DRAINAGE STRUCTURES. ONLY ALTERNATE 1 AND 3 MAY BE USED FOR CATTLE PASSES, UNLESS OTHERWISE STATED IN THE CONTRACT. THE MATERIALS, FABRICATION AND WORK NECESSARY TO TIE THE PIPE BY THIS DETAIL WILL BE CONSIDERED INCIDENTAL TO THE PIPE AND APRON ENDWALLS IF REQUIRED.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR JOINT TIES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.

JOINT TIES TO BE HOT-DIP GALVANIZED PER ASTM A 153.

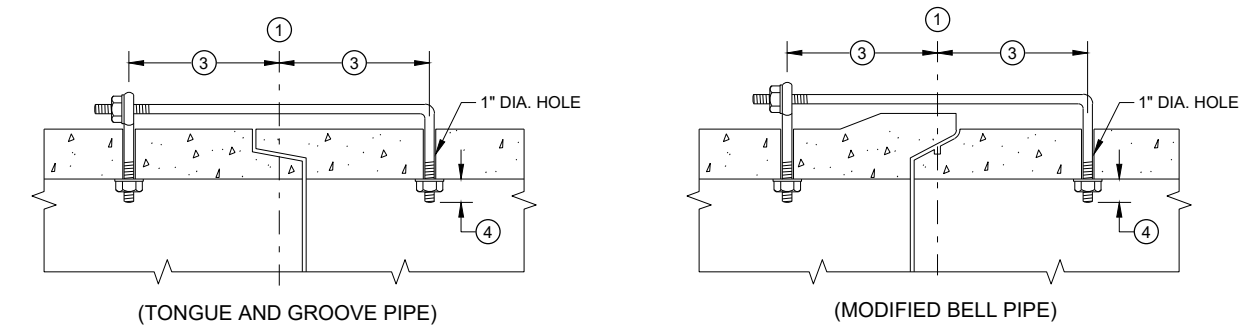
- ① CENTER LINE OF TONGUE AND GROOVE OR BELL AND SPIGOT JOINTS.
- ② THE INSIDE OF THE THREADED INSERTS SHALL BE CLEAN TO ALLOW THE INSERTION OF THREADED EYE BOLTS.
- ③ HOLES SHALL BE CAST-IN-PLACE OR DRILLED PER THE APPLICABLE DETAIL, AND EQUAL DISTANCE FROM THE CENTERLINE OF THE JOINT.
- ④ BOLT PROJECTION INSIDE OF PIPE SHALL NOT EXCEED 2 INCHES.
- ⑤ OPENING TO BE ROD DIAMETER PLUS 1 INCH.
- ⑥ LENGTH ADEQUATE TO EXTEND TO WITHIN 1/2 INCH OF THE INNER SURFACE OF THE PIPE.
- ⑦ EYE BOLT LENGTH DETERMINED BY WALL THICKNESS, BELL THICKNESS AND BOLT PROJECTION INSIDE PIPE.



**EYE BOLT** ⑦

NOTE: TWO EYE BOLTS MAY BE USED WITH A 30\"/>

**EYE BOLT AND TIE ROD**



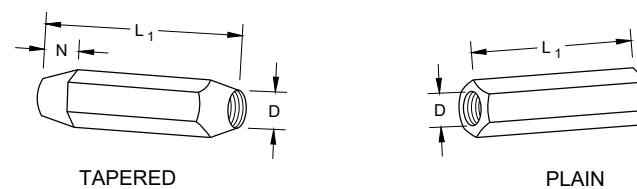
**LONGITUDINAL SECTION**  
(JOINT TIES FOR 18\"/>

**EYE BOLT AND TIE ROD ASSEMBLY (ALTERNATE NO. 2)**

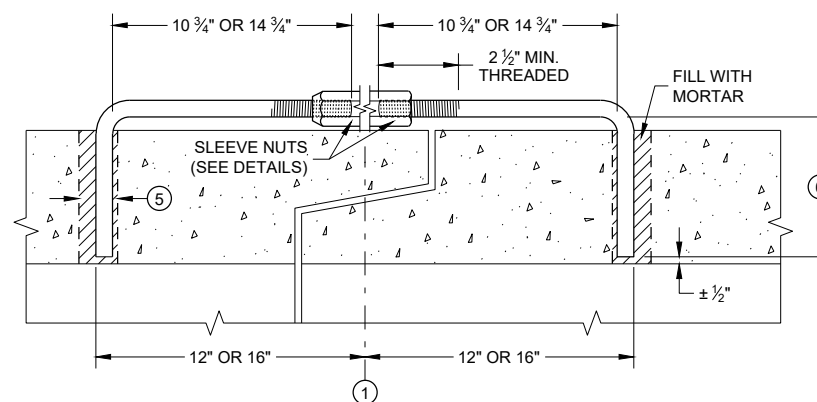
**ADJUSTABLE TIE ROD TABLE**

PIPE DIAMETER	TIE ROD DIAMETER	D	L <sub>1</sub>	N
12 - 60	5/8	5/8	5	1/2
66 - 84	3/4	3/4	5	1/2
90 - 144	1	1	7	1 7/16

DIMENSIONS SHOWN ARE IN INCHES

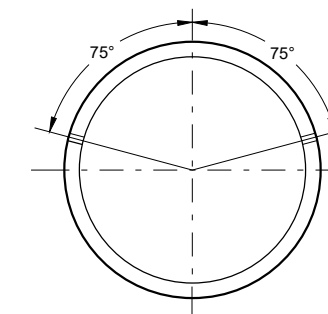


**RIGHT AND LEFT THREADS SLEEVE NUTS**



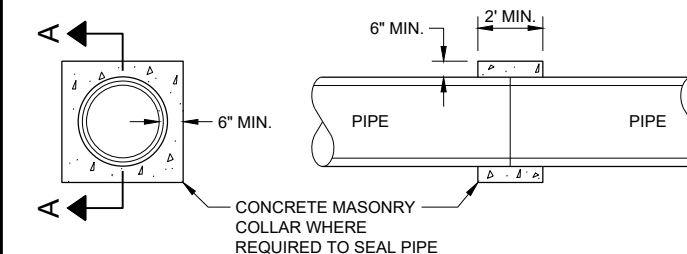
**LONGITUDINAL SECTION**

**ADJUSTABLE TIE ROD (ALTERNATE NO. 3)**



PLACEMENT OF (2) CAST-IN-PLACE INSERTS OR HOLES DURING FABRICATION FOR PIPE SECTIONS REQUIRING TIE RODS

**TRANSVERSE SECTION**



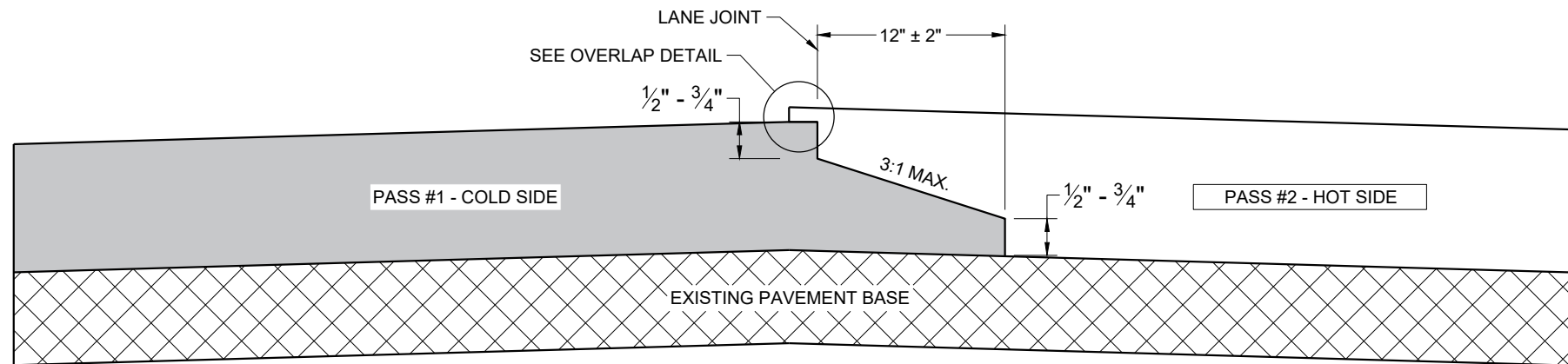
**SECTION A - A**  
**CONCRETE COLLAR DETAIL**

**JOINT TIES FOR CONCRETE PIPE AND CONCRETE COLLAR DETAIL**

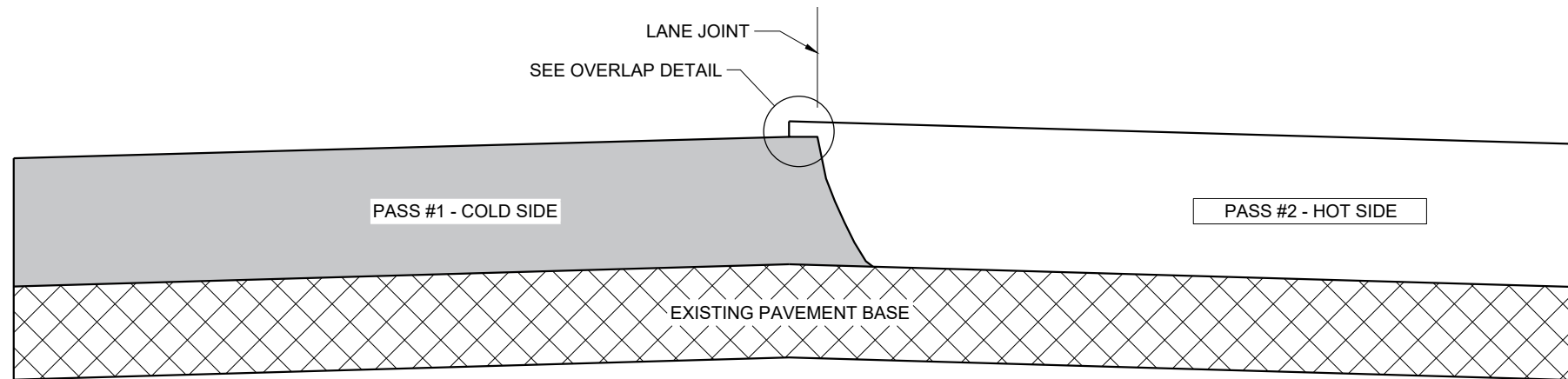
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
November 2021 /S/ Rodney Taylor  
DATE ROADWAY STANDARDS DEVELOPMENT  
ENGINEER

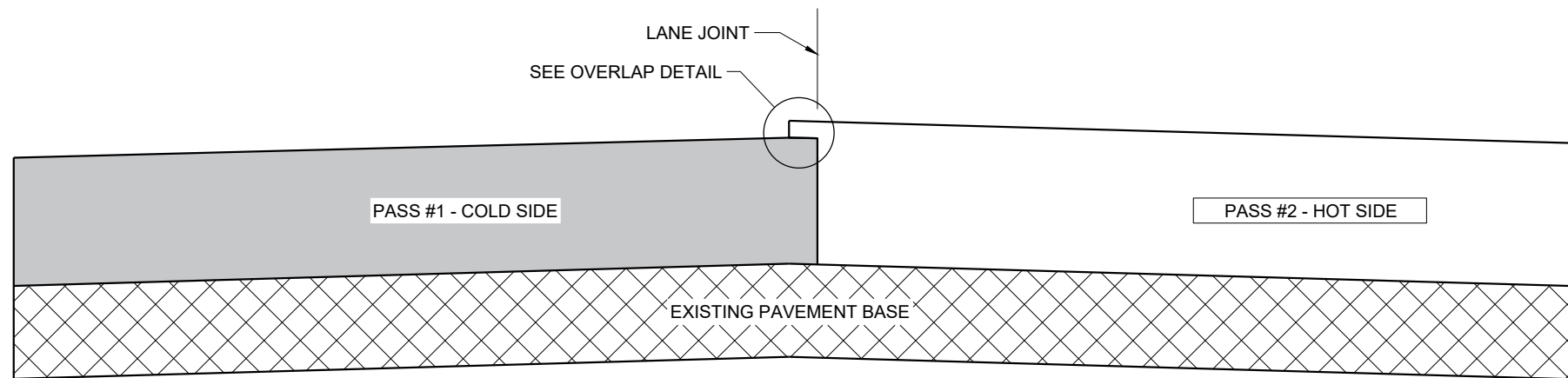
FHWA



**TYPICAL PAVEMENT CROSS SECTION  
NOTCHED WEDGE JOINT**



**TYPICAL PAVEMENT CROSS SECTION  
VERTICAL JOINT**



**TYPICAL PAVEMENT CROSS SECTION  
VERTICAL JOINT (MILLED)**

**GENERAL NOTES**

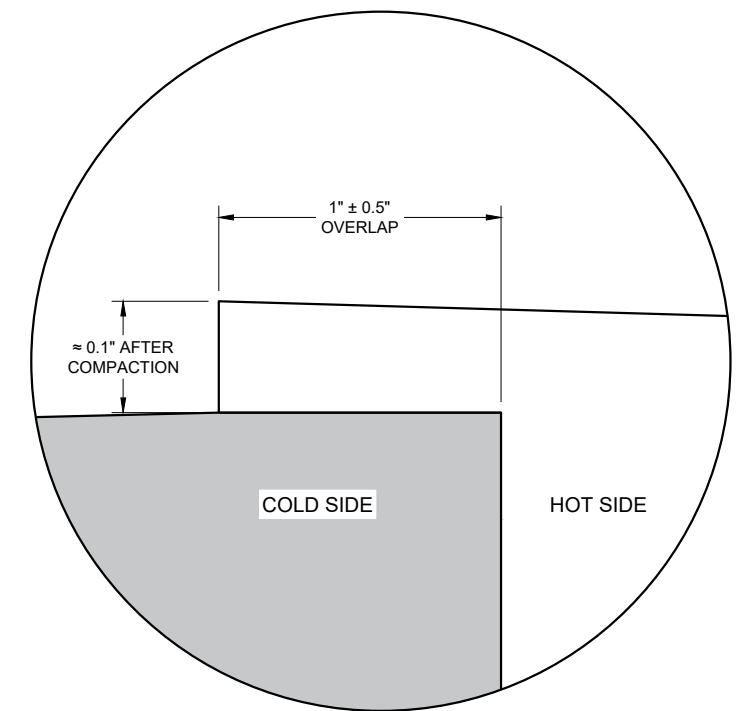
IN ADDITION TO THE DETAILS PROVIDED IN THIS DRAWING, CONFORM TO STANDARD SPECIFICATION 450.3.2.8 FOR WHEN A NOTCHED WEDGE JOINT IS REQUIRED AND FOR GENERAL JOINT CONSTRUCTION REQUIREMENTS.

FOR ALL LONGITUDINAL JOINTS, ENSURE THE PAVER SCREED OVERLAPS THE PREVIOUSLY PLACED PAVEMENT BY  $1" \pm 0.5"$  AND THE HOT SIDE OF THE JOINT REMAINS HIGHER THAN THE COLD SIDE BY APPROXIMATELY  $0.1"$  AFTER FINAL COMPACTION. (IT WILL BE FLUSH WHEN PAVING IN ECHELON.)

ONLY REMOVE THE LONGITUDINAL NOTCHED WEDGE JOINT FOR SMA PAVEMENT OR AS DIRECTED BY THE ENGINEER TO ADDRESS SPECIFIC LENGTHS OF JOINT DAMAGED BY TRAFFIC.

WHEN MILLING BACK OR REMOVING ANY LONGITUDINAL JOINT, LIMIT THE MATERIAL REMOVED TO  $2"$  FROM THE TOP NOTCH OR FROM THE VERTICAL JOINT EDGE ON THE COLD SIDE OF THE JOINT.

USE LONGITUDINAL MILLED JOINT AS PLANS SHOW OR THE AS THE ENGINEER DIRECTS.



**OVERLAP DETAIL (TYPICAL)**

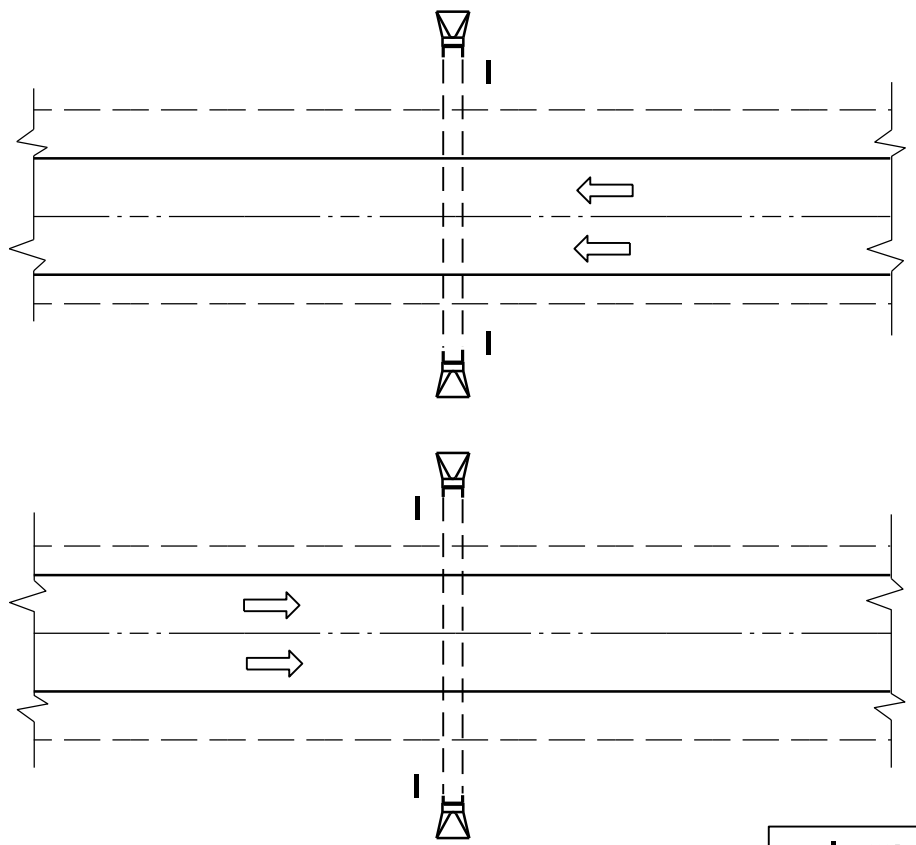
6

6

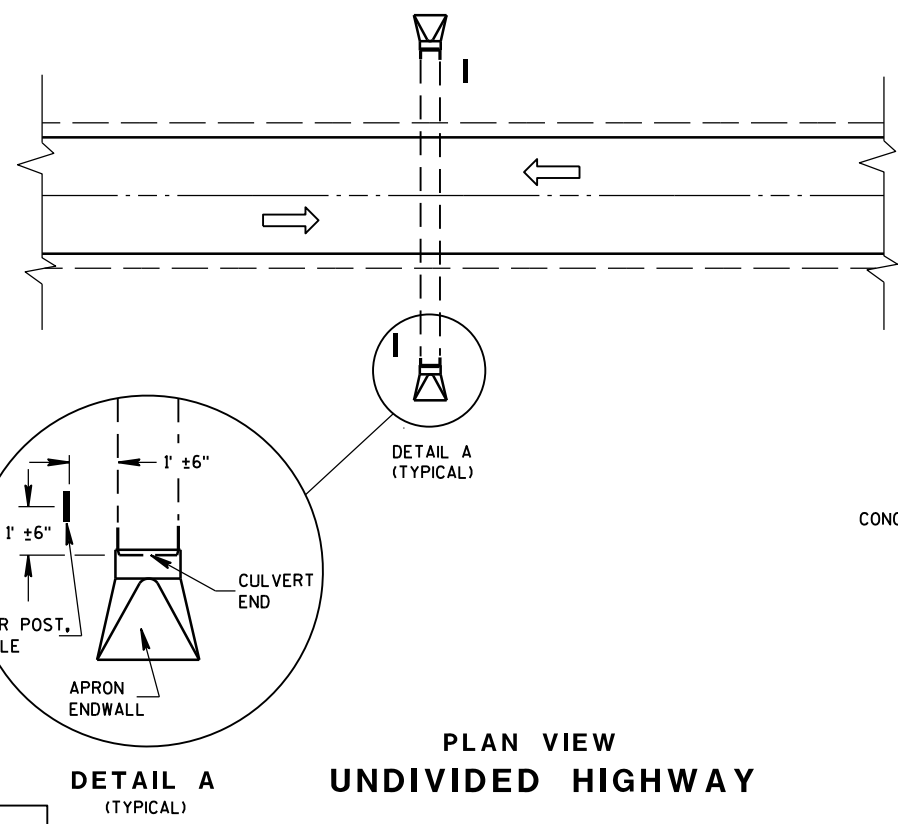
SDD 13C19 - 03

SDD 13C19 - 03

<b>HMA LONGITUDINAL JOINTS</b>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED November 2020 DATE	/S/ Steven Hefel HMA PAVEMENT ENGINEER
FHWA	



PLAN VIEW  
DIVIDED HIGHWAY



PLAN VIEW  
UNDIVIDED HIGHWAY

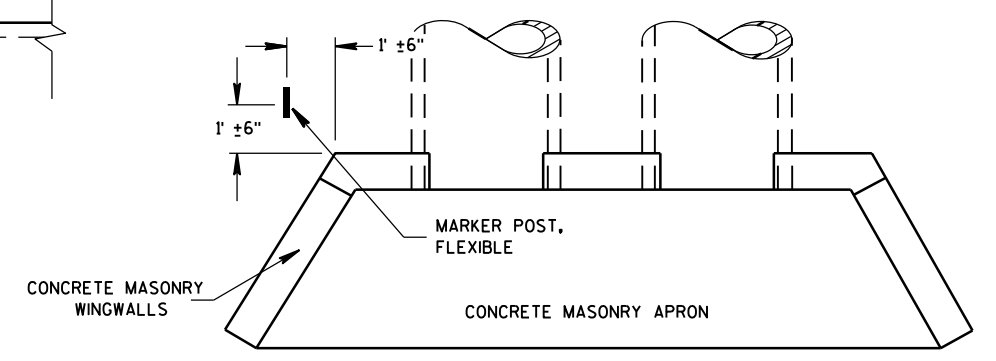
MARKER POST, FLEXIBLE  
DIRECTION OF TRAFFIC FLOW

DETAIL A  
(TYPICAL)

FLEXIBLE MARKER POST LOCATION

GENERAL NOTES

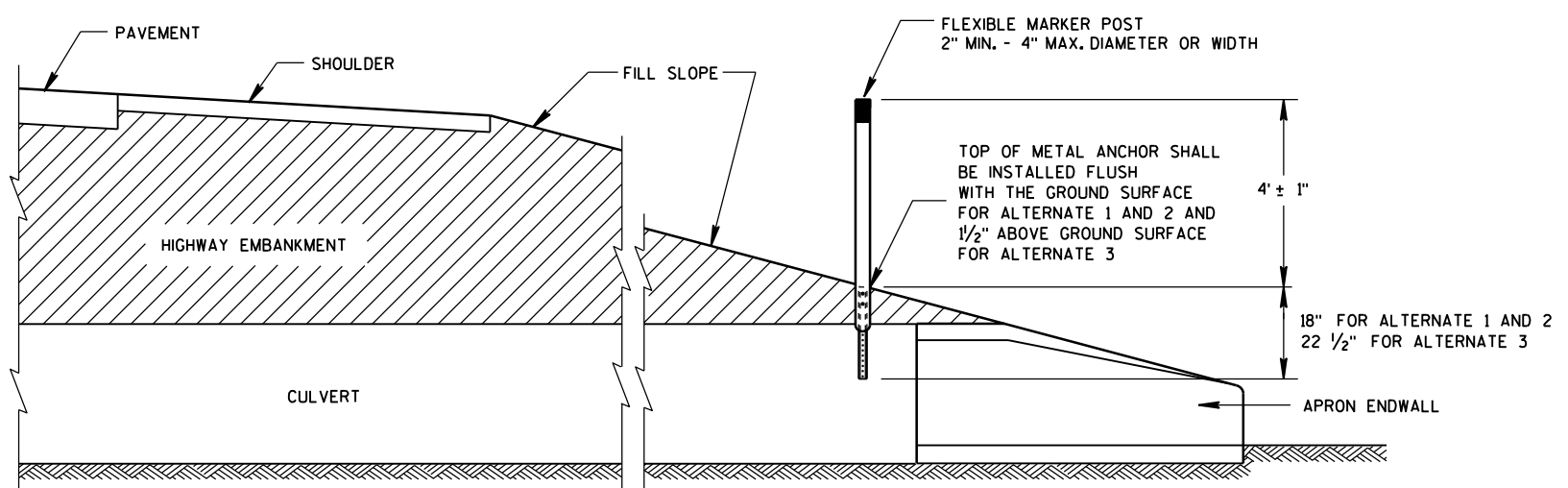
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.



PLAN VIEW  
CONCRETE MASONRY ENDWALLS FOR  
CULVERT PIPE AND PIPE ARCH

6

6



CROSS SECTION  
FLEXIBLE MARKER POST

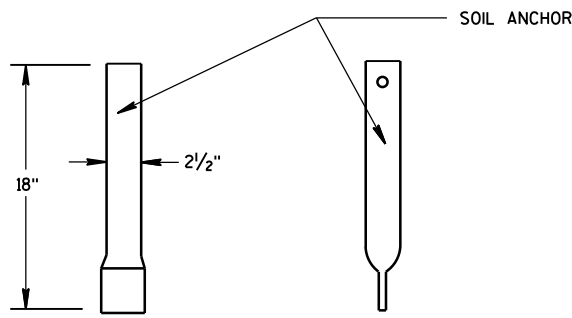
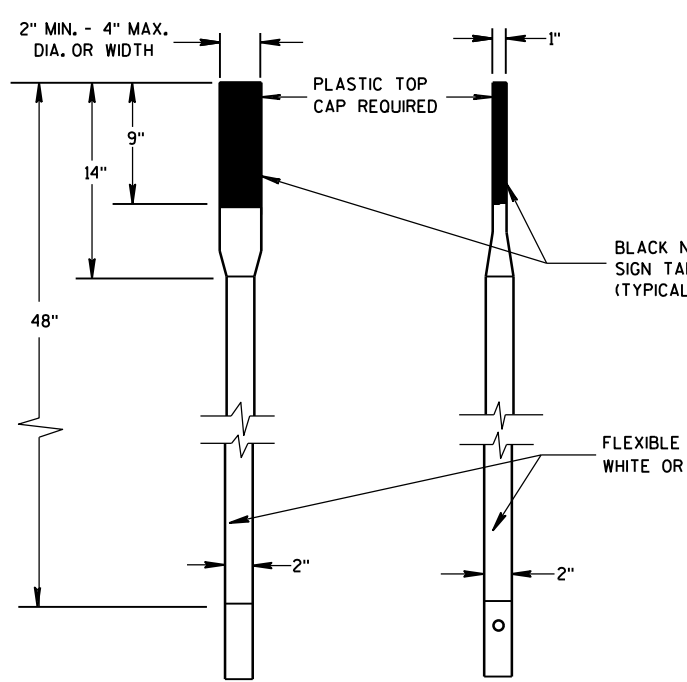
FLEXIBLE MARKER POST  
FOR CULVERT END

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

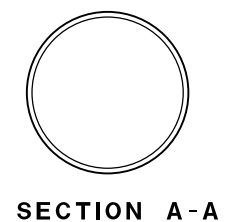
S.D.D. 15 A 3-2a

S.D.D. 15 A 3-2a

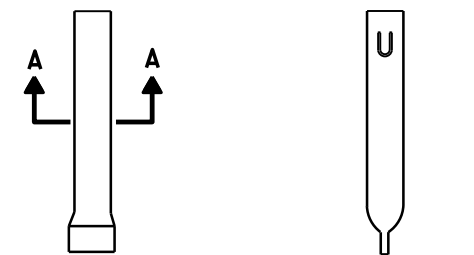




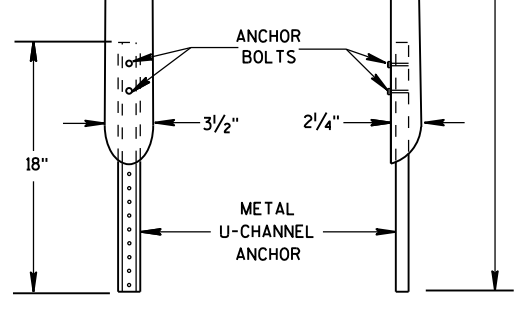
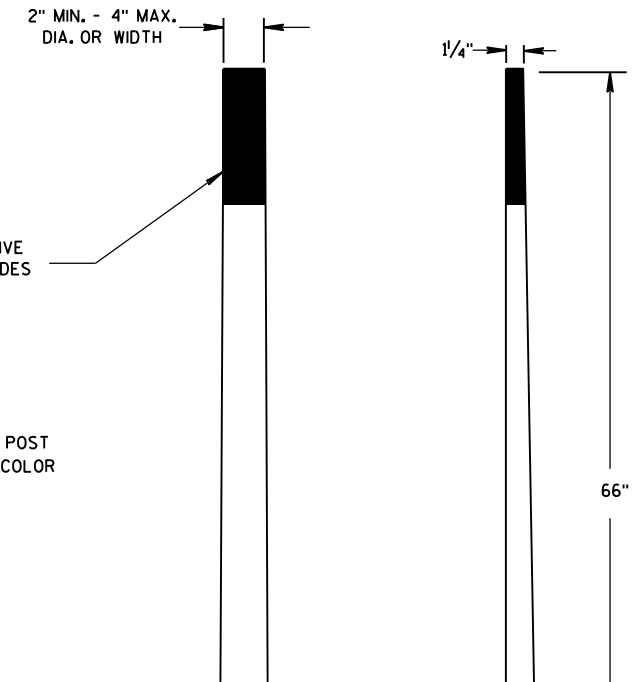
FRONT VIEW SIDE VIEW  
ALTERNATE 1



SECTION A-A

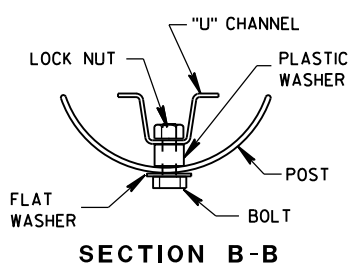


FRONT VIEW SIDE VIEW  
ALTERNATE 1

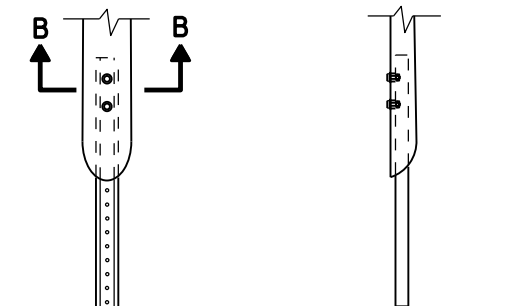


FRONT VIEW SIDE VIEW  
ALTERNATE 2

**FLEXIBLE MARKER POSTS**

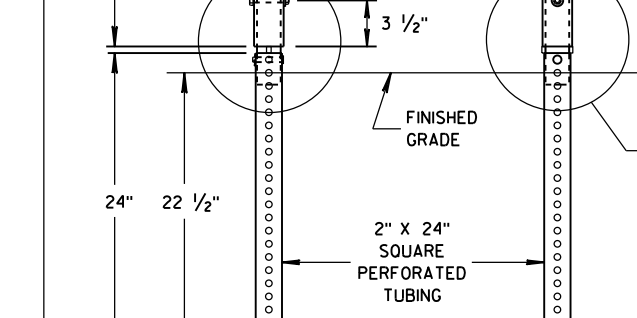
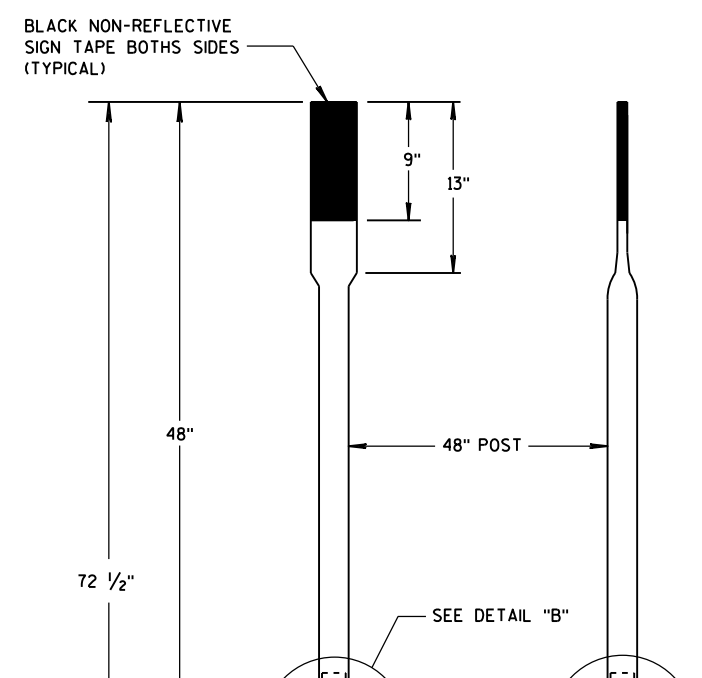


SECTION B-B

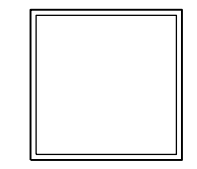


FRONT VIEW SIDE VIEW  
ALTERNATE 2

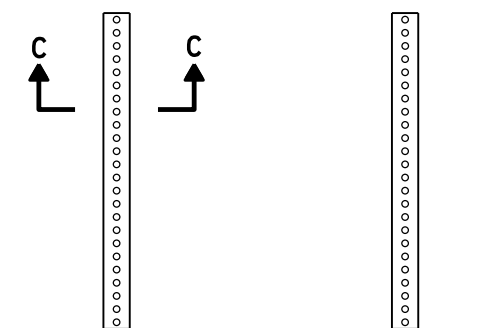
**FLEXIBLE MARKER POST ANCHORS**



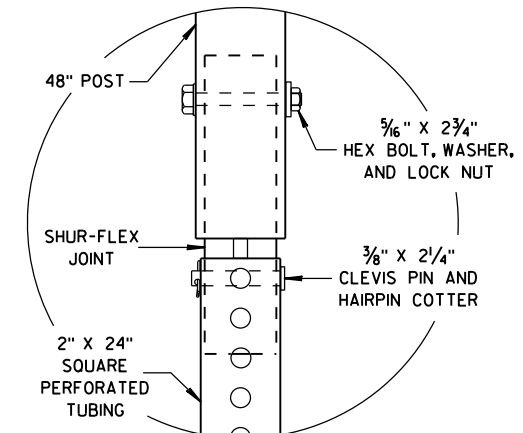
FRONT VIEW SIDE VIEW  
ALTERNATE 3



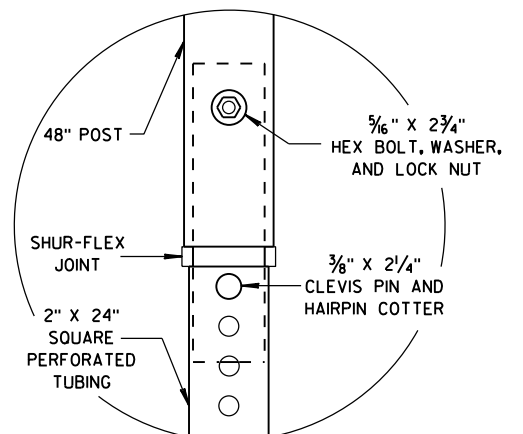
SECTION C-C



FRONT VIEW SIDE VIEW  
ALTERNATE 3



DETAIL B

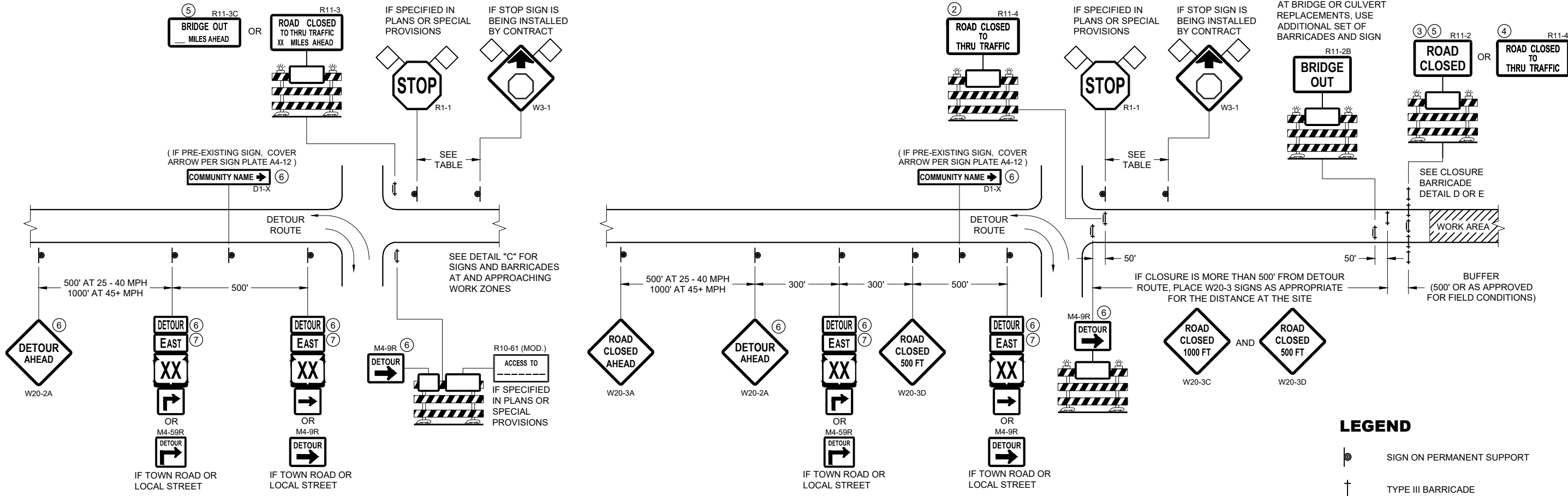


DETAIL C

**FLEXIBLE MARKER POST FOR CULVERT END**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
10/1/2012 DATE /S/ Travis Feltes  
STATE TRAFFIC ENGINEER OF DESIGN  
FHWA



**DETAIL A  
MAINLINE CLOSURE WITH POSTED DETOUR**

WORK ZONE GREATER THAN OR EQUAL TO 1/2 MILE FROM  
DETOUR ROUTE ( 1000 FEET IF URBAN )

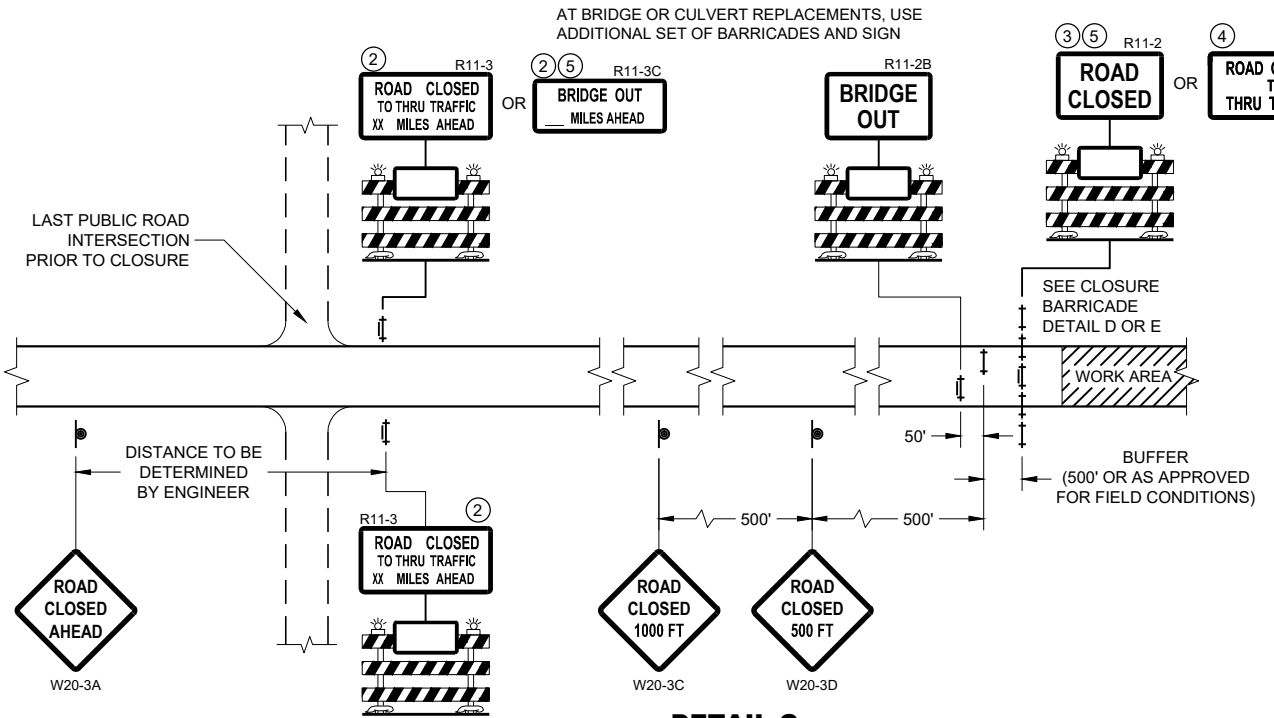
**DETAIL B  
MAINLINE CLOSURE WITH POSTED DETOUR**

WORK ZONE LESS THAN 1/2 MILE FROM  
DETOUR ROUTE ( 1000 FEET IF URBAN )

- LEGEND**
- SIGN ON PERMANENT SUPPORT
  - TYPE III BARRICADE
  - TYPE III BARRICADE WITH ATTACHED SIGN
  - TYPE "A" WARNING LIGHT (FLASHING)
  - WORK AREA
  - FLAGS, 16" X 16" MIN. (ORANGE)

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

- M4 - 8
- M3 - X
- M1 - 4
- M1 - 6
- M1 - 5A
- M05 - 1
- M06 - 1



**DETAIL C  
MAINLINE CLOSURE, NO POSTED DETOUR**

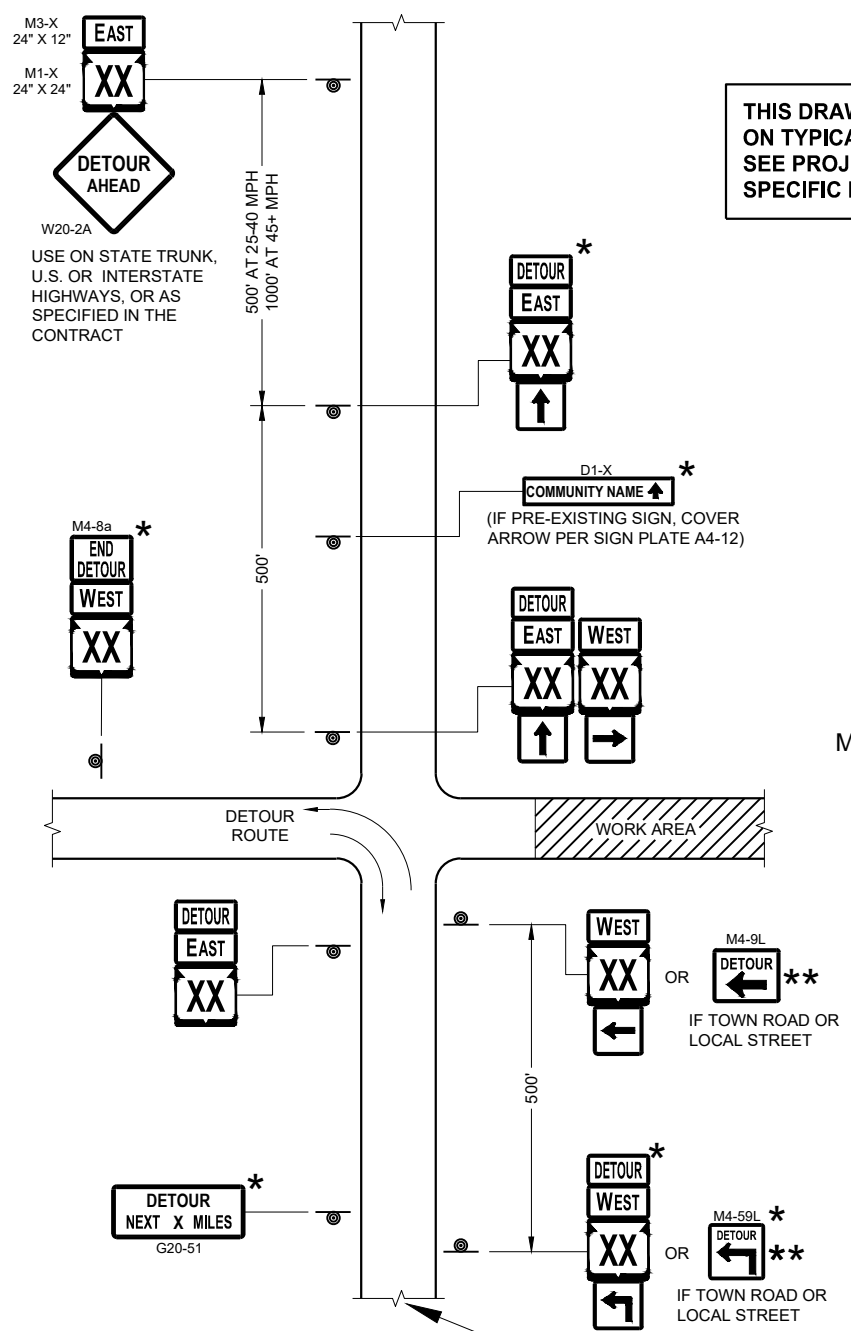
SEE SDD 15C2-SHEET "b"  
FOR GENERAL NOTES  
AND FOOTNOTES ① THROUGH ⑦

**BARRICADES AND SIGNS  
FOR MAINLINE CLOSURES**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
May 2023 /S/ Andrew Heidtke  
DATE DATE WORK ZONE ENGINEER

FHWA



THIS DRAWING PROVIDES GENERAL GUIDANCE ON TYPICAL DETOUR SIGN LAYOUT AND SPACING. SEE PROJECT DETOUR SIGNING SHEETS FOR SPECIFIC DETAILS FOR EACH PROJECT.

**LEGEND**

- SIGN ON PERMANENT SUPPORT
- WORK AREA
- M4 - 8
- M3 - X
- M1 - 4
- M1 - 6
- M1 - 5A
- M05 - 1
- M06 - 1
- M06 - 1

**GENERAL NOTES**

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. MODIFY EXISTING SIGNS WHERE POSSIBLE.

THE SPACING BETWEEN TRAFFIC CONTROL AND DETOUR SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

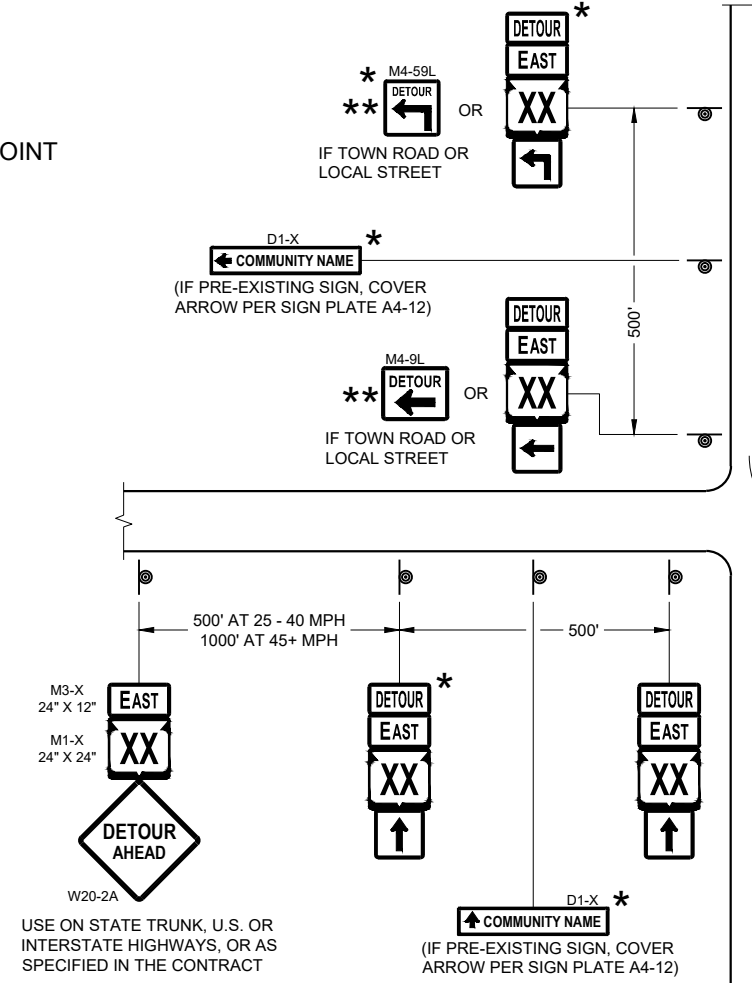
"MO" SIGNS ARE THE SAME AS "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGN SIZES SHALL BE AS FOLLOWS:

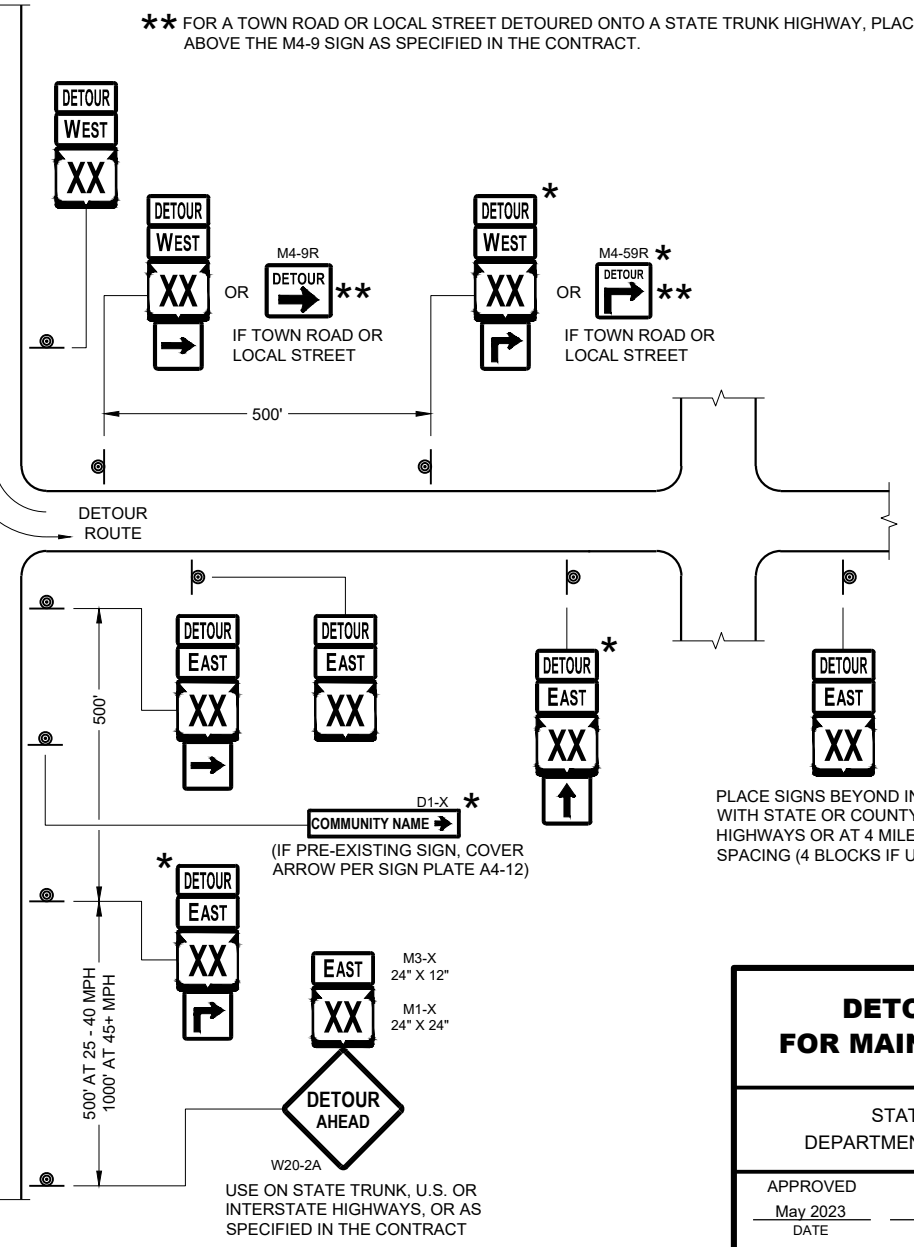
- M3-X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4-8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1-4, M1-5A AND M1-6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- M05-1 AND M06-1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- M4-9 AND M4-9R SHALL BE 30" X 24"
- M4-8a SHALL BE 24" X 18"
- G20-51 SHALL BE 60" X 24"
- W20-2A SHALL BE 48" X 48"
- D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.

- \* OPTIONAL SIGNS. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS.
- \*\* FOR A TOWN ROAD OR LOCAL STREET DETOURED ONTO A STATE TRUNK HIGHWAY, PLACE A ROAD NAME PLAQUE ABOVE THE M4-9 SIGN AS SPECIFIED IN THE CONTRACT.

MATCH POINT



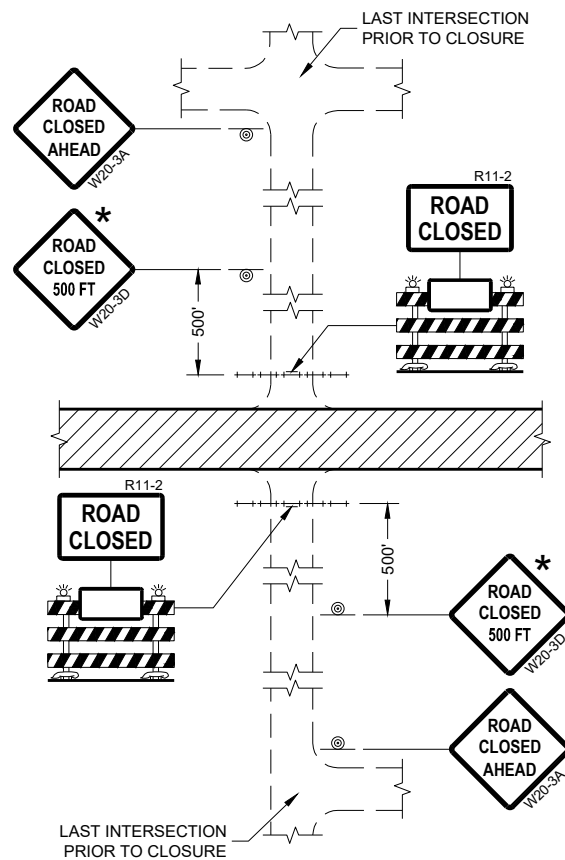
**DETAIL F  
DETOUR SIGNING**



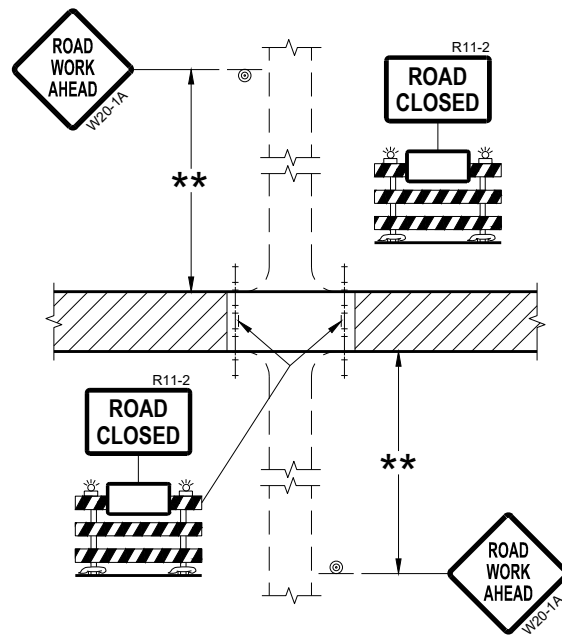
PLACE SIGNS BEYOND INTERSECTIONS WITH STATE OR COUNTY TRUNK HIGHWAYS OR AT 4 MILE MAXIMUM SPACING (4 BLOCKS IF URBAN AREA)

SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS AND DETAIL A OR B ON SDD SHEET 15C02 - SHEET "a"

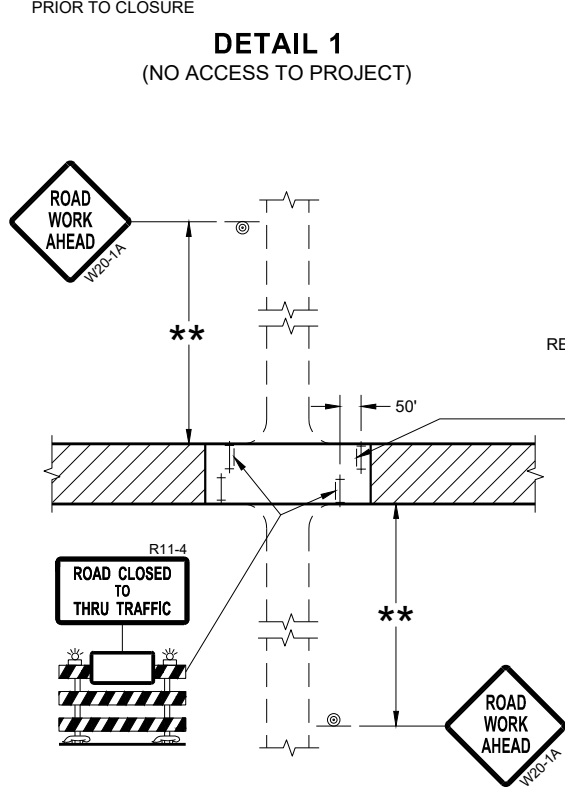
DETOUR SIGNING FOR MAINLINE CLOSURES	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2023 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	



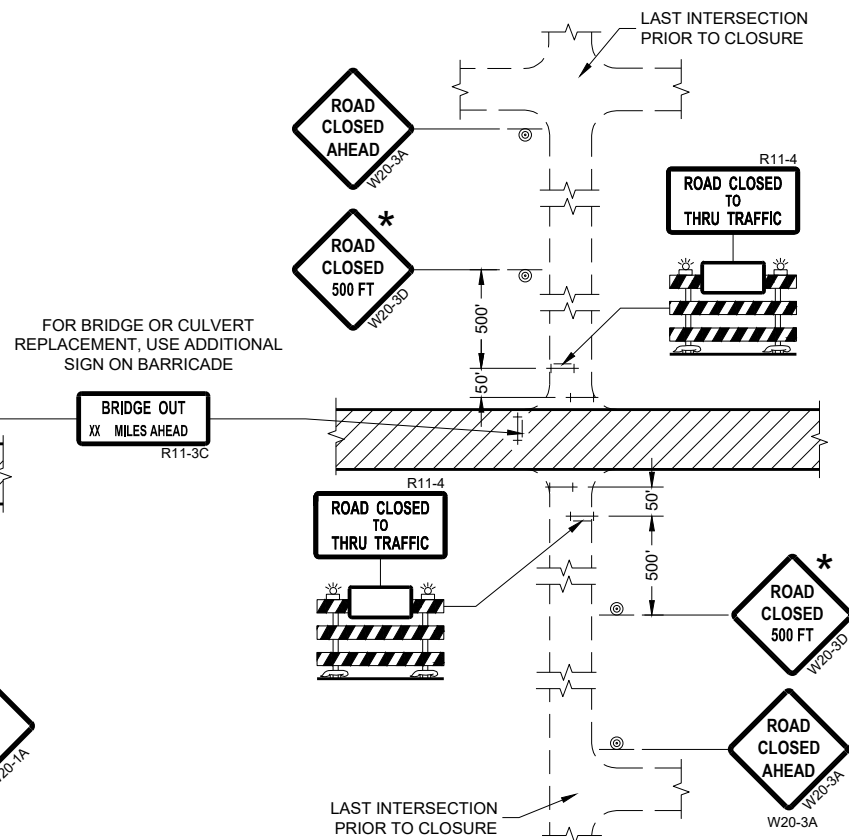
**DETAIL 1**  
(NO ACCESS TO PROJECT)



**DETAIL 2**  
(PUBLIC CROSS-TRAFFIC MAINTAINED.  
NO ACCESS TO PROJECT)



**DETAIL 3**  
(PUBLIC CROSS-TRAFFIC MAINTAINED.  
CONTRACTOR, LOCAL BUSINESS AND  
RESIDENT ACCESS TO PROJECT)



**DETAIL 4**  
(CONTRACTOR, LOCAL BUSINESS AND  
RESIDENT ACCESS TO PROJECT)

**GENERAL NOTES**

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE (500 FEET DESIRABLE) TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3, AND R11-4 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:  
R11-2 SHALL BE 48" X 30".  
R11-4 AND R11-3 SHALL BE 60" X 30".

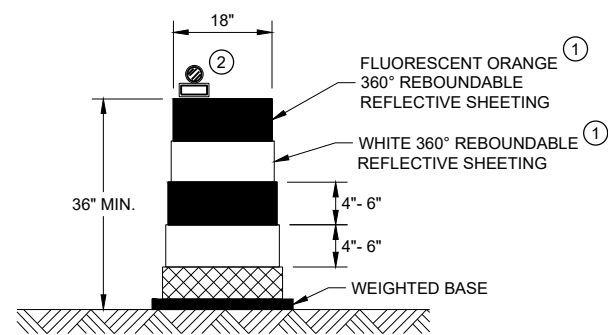
- \* OMIT THE "ROAD CLOSED 500 FT." SIGN IF THE LAST INTERSECTION IS 500 FEET OR LESS FROM THE WORK ZONE.
- \*\* 500' MAX. OR AT LAST INTERSECTION, WHICHEVER IS CLOSEST.

**LEGEND**

- ⊙ SIGN ON PERMANENT SUPPORT
- ⊥ TYPE III BARRICADE
- ⊥ TYPE III BARRICADE WITH ATTACHED SIGN
- ⚡ TYPE "A" WARNING LIGHT (FLASHING)
- ▨ WORK AREA

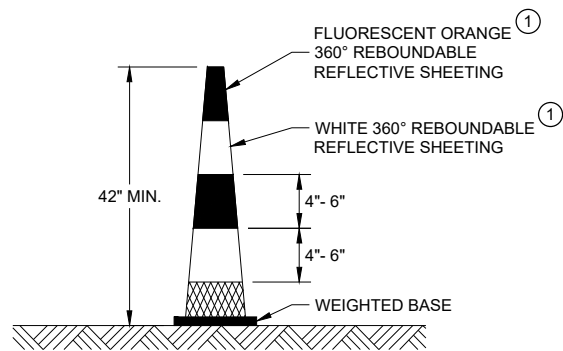
<b>BARRICADES AND SIGNS FOR SIDEROAD CLOSURES</b>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED July 2018 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
<small>FHWA</small>	





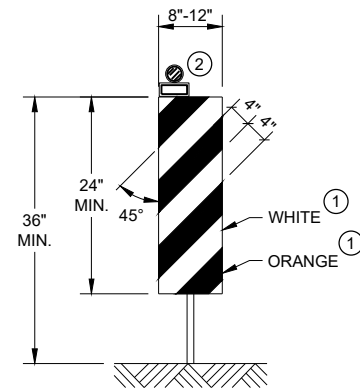
**DRUM**

BALLAST WIDTHS  
RANGE FROM 24"-36"



**42" CONE**

DO NOT USE IN TAPERS  
½ SPACING OF DRUMS  
BALLAST WIDTHS  
RANGE FROM 14"-20"

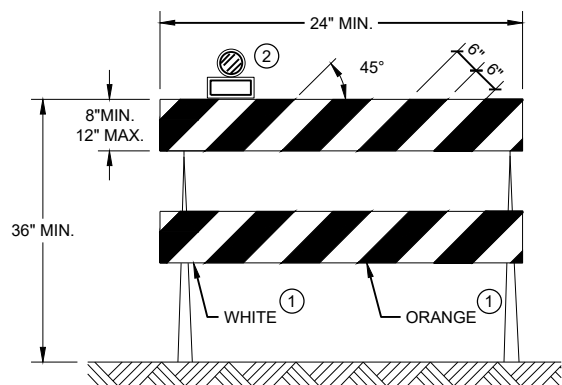


**VERTICAL PANEL**

THE STRIPES SHALL SLOPE DOWNWARD TO  
THE TRAFFIC SIDE FOR CHANNELIZATION.

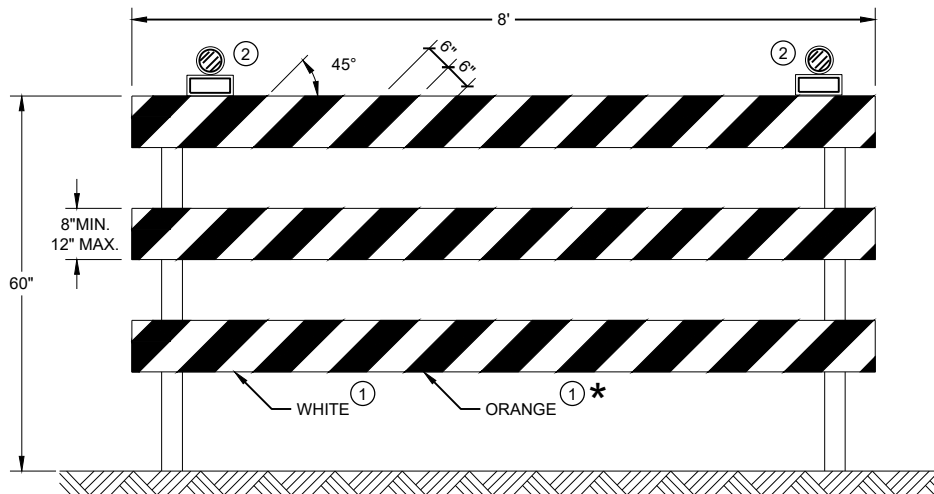
**GENERAL NOTES**

- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.



**TYPE II BARRICADE**

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES  
MAY BE USED. ALL STRIPES SHALL SLOPE DOWNWARD  
TO THE TRAFFIC SIDE FOR CHANNELIZATION.



**TYPE III BARRICADE**

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP  
TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.

\* IF USED FOR A PERMANENT APPLICATION USE RED SHEETING.

**CHANNELIZING DEVICES  
DRUMS, CONES, BARRICADES  
AND VERTICAL PANELS**

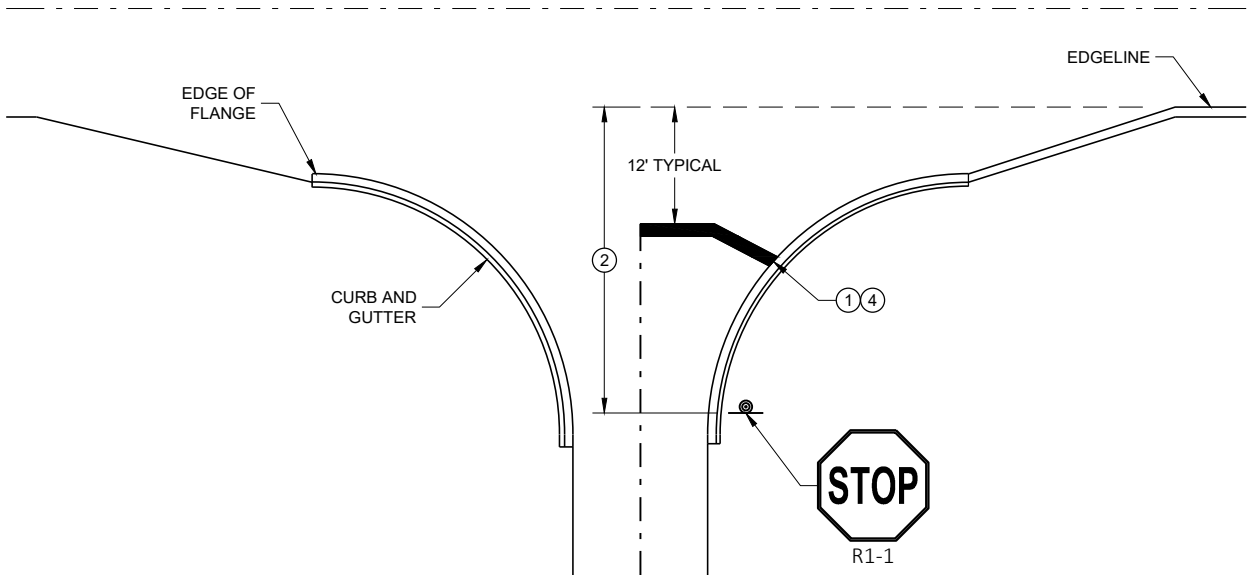
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
 November 2022 /S/ Andrew Heidtke  
 DATE WORK ZONE ENGINEER  
 FHWA

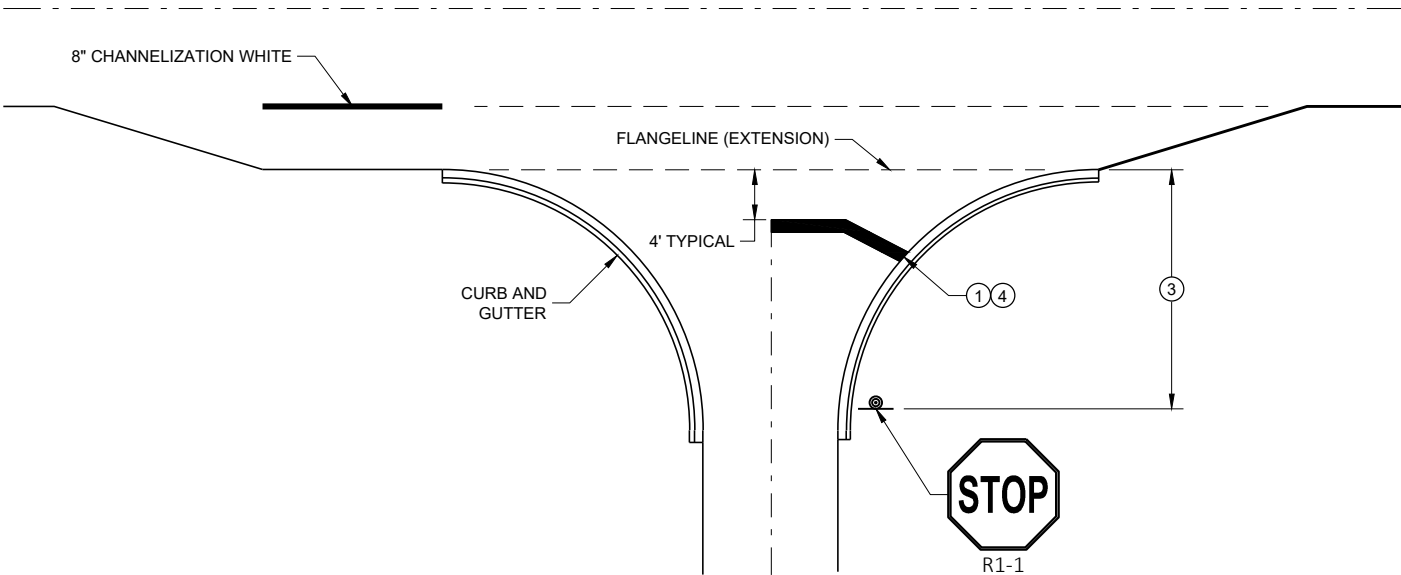
**GENERAL NOTES**

STOP SIGN SHALL BE PLACED A MINIMUM OF 6 FEET TO A MAXIMUM OF 50 FEET FROM THE EDGELINE LOCATION.

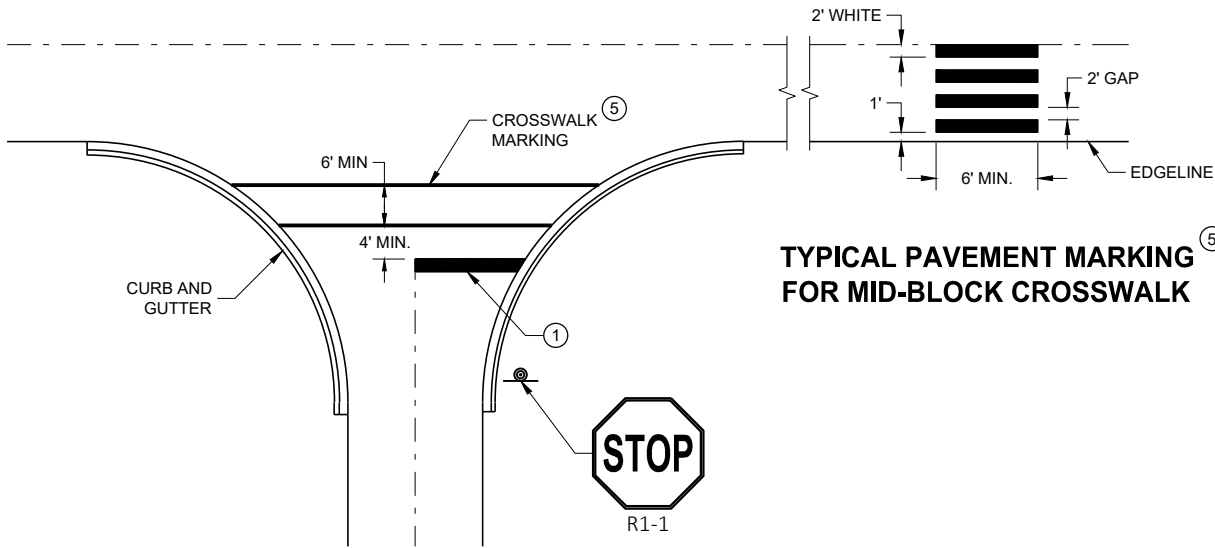
- ① 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE REGION MARKING ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
- ② NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGELINE.
- ③ NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGELINE EXTENSION.
- ④ MOVE CLOSER TO THE EDGE OF TRAVEL LINE AS NEEDED FOR VISIBILITY AND SIGHT LINES (NO CLOSER THAN 4 FEET).
- ⑤ LADDER BAR CROSSWALKS SHOULD ONLY BE USED FOR MID BLOCK CROSSINGS. USE 2 - 6" TRANSVERSE LINES INSTEAD.



**TYPICAL STOP LINE PAVEMENT MARKING WITH CURB AND GUTTER**

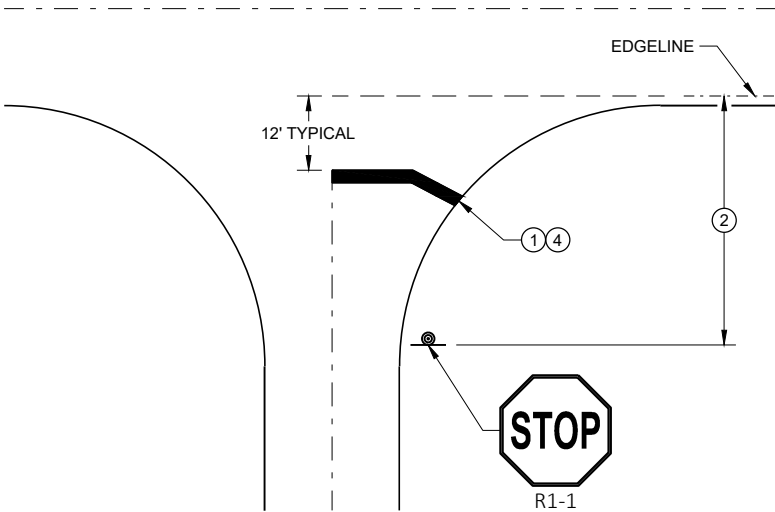


**TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH RIGHT TURN LANE**



**TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH CROSSWALK MARKING**

**TYPICAL PAVEMENT MARKING FOR MID-BLOCK CROSSWALK**



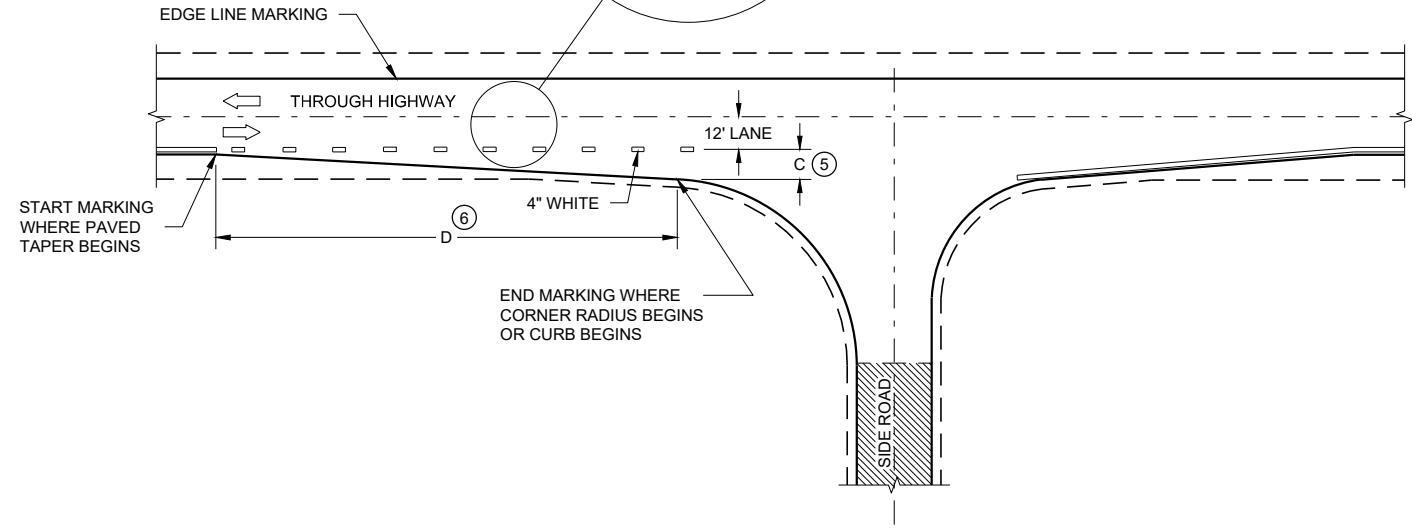
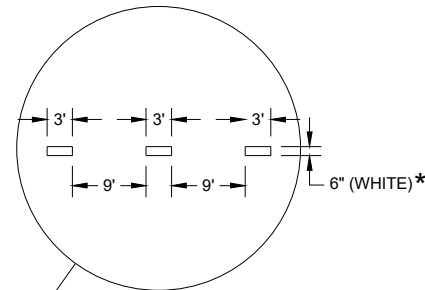
**TYPICAL STOP LINE PAVEMENT MARKING WITHOUT CURB AND GUTTER**

**STOP LINE AND CROSSWALK PAVEMENT MARKING**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
November 2019 /S/ Matthew Rauch  
DATE STATE SIGNING AND MARKING ENGINEER

FHWA



**MINOR INTERSECTION**

\* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES

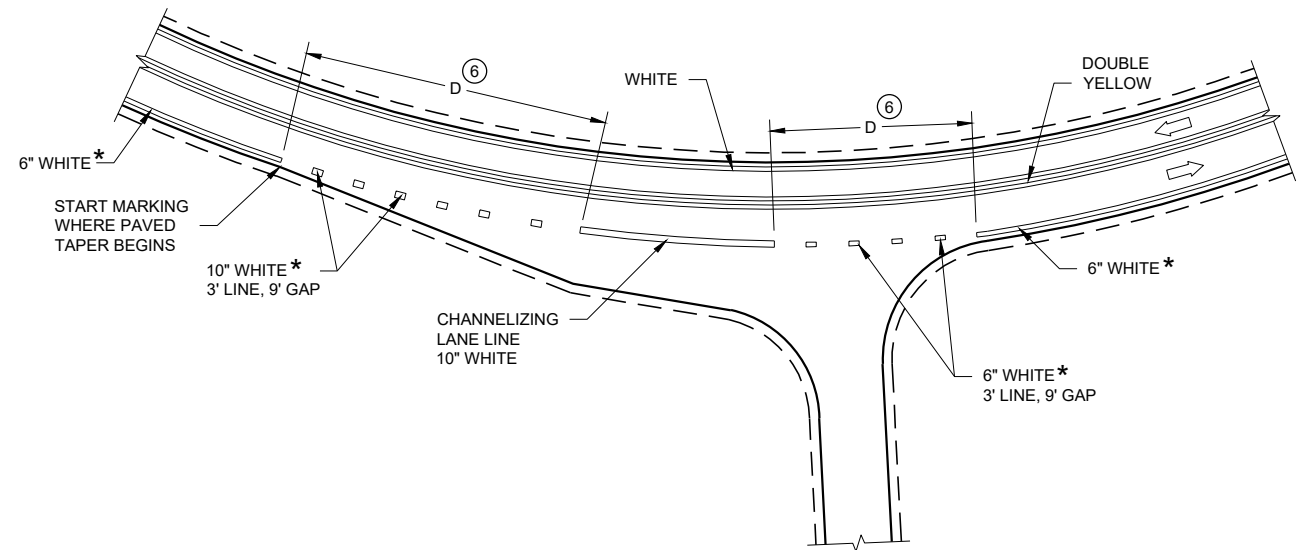
**GENERAL NOTES**

OMIT EDGE LINES THROUGH INTERSECTIONS. CONTINUE EDGE LINES THROUGH DRIVEWAYS.

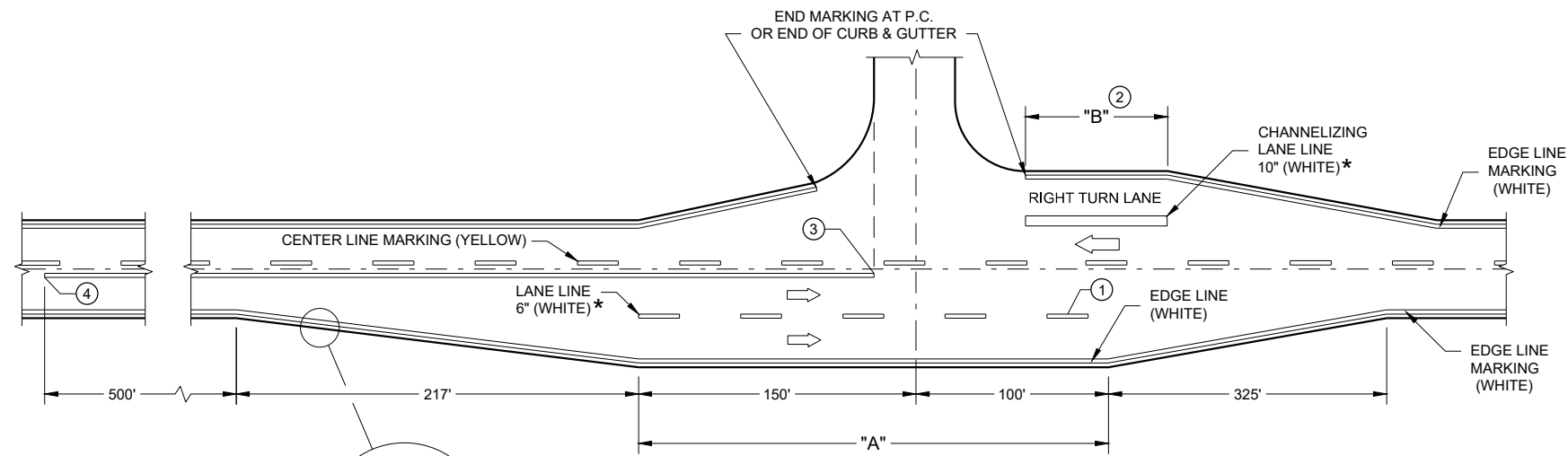
- ① WHEN DISTANCE "A" IS LESS THAN 250 FEET, OMIT LANE LINE.
- ② WHEN DISTANCE "B" IS LESS THAN 100 FEET, OMIT CHANNELIZING LANE LINE.
- ③ BARRIER LINE ENDS AT SIDE ROAD PAVEMENT / SURFACE EDGE EXTENSION.
- ④ BARRIER LINE STARTS 500 FEET PRIOR TO THE BYPASS TAPER.
- ⑤ WHEN DISTANCE "C" IS LESS THAN 4 FEET, OMIT DOTTED EXTENSION.
- ⑥ WHEN DISTANCE "D" IS LESS THAN 50 FEET, OMIT DOTTED EXTENSION.

**LEGEND**

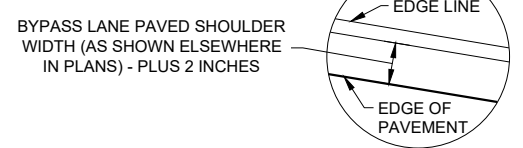
➔ DIRECTION OF TRAVEL



**INTERSECTION ON OUTSIDE OF CURVE**



**MAJOR INTERSECTIONS  
(INTERSECTION WITH FULL RIGHT TURN LANE OR BYPASS LANE)**







**PAVEMENT MARKING  
(INTERSECTIONS)**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION



**LEGEND**

-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  DIRECTION OF TRAFFIC
-  WORK ZONE

**GENERAL NOTES**

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" SIGNS MAY BE USED IF APPROVED BY THE REGIONAL TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

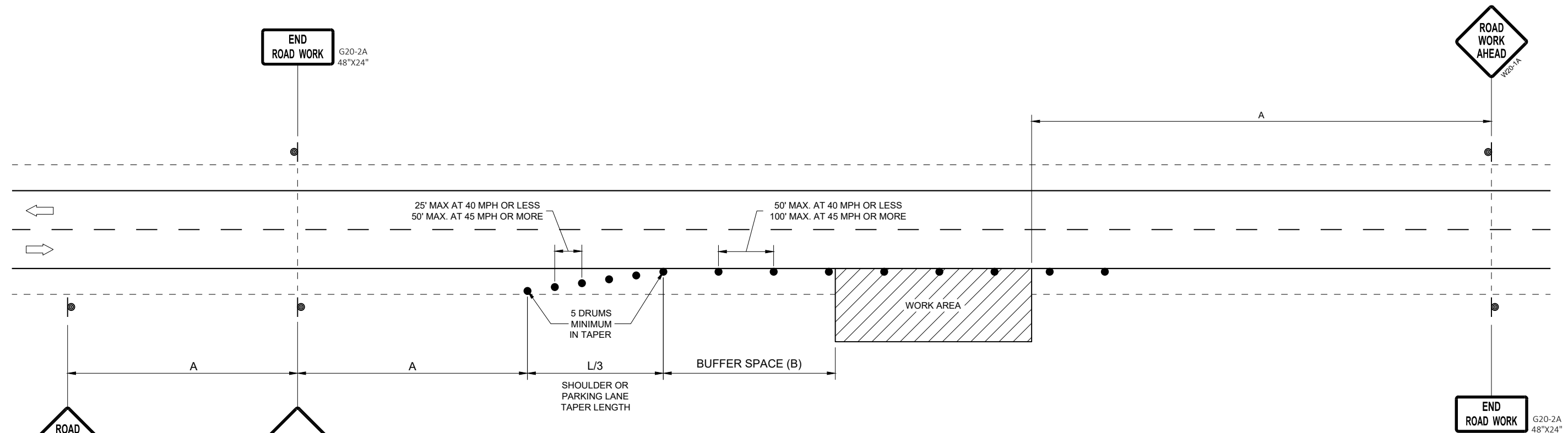
CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

W20-1A AND G20-2A SIGNS ARE NOT REQUIRED IF THE WORK AREA IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT. G20-2A SIGNS MAY ALSO BE OMITTED IF DURATION OF WORK IS LESS THAN 7 CONTINUOUS DAYS AND NIGHTS.

6

6



POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET	SHOULDER TAPER L / 3 W, LATERAL OFFSET (FT)						BUFFER SPACE (B) FEET
		3	4	5	6	7	8	
25	200'	10	14	17	21	24	28	55
30	200'	15	20	25	30	35	40	85
35	350'	20	27	34	40	47	54	120
40	350'	26	35	44	53	62	70	170
45	500'	45	59	74	89	104	119	220
50	500'	50	66	83	99	116	132	280
55	500'	54	73	91	109	127	145	335'

**TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

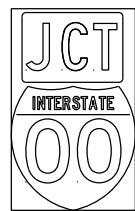
APPROVED  
May 2020 /S/ Andrew Heidtke  
DATE STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER

FHWA

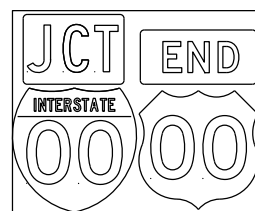
SDD 15D28 - 04

SDD 15D28 - 04

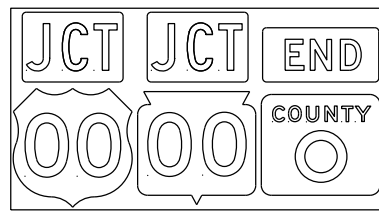
# TYPICAL ASSEMBLIES



J1-1



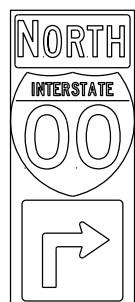
J1-2



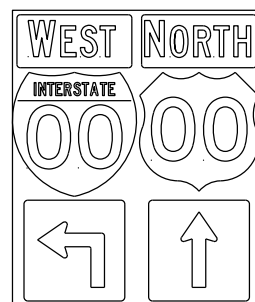
J1-3



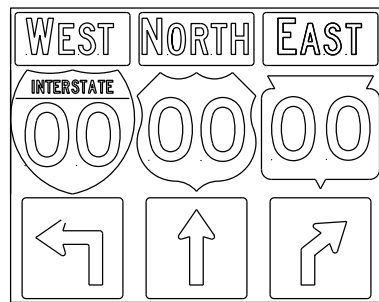
JR1-1



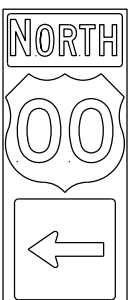
J2-1



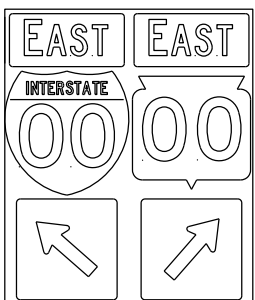
J2-2



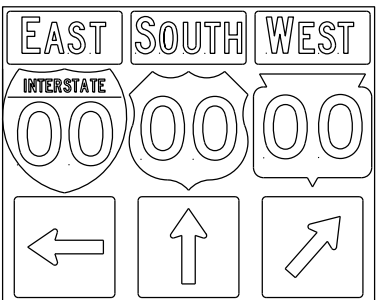
J2-3



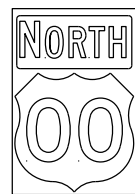
J3-1



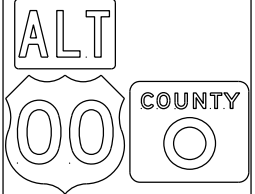
J3-2



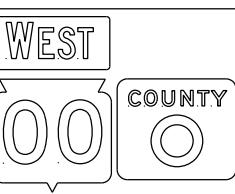
J3-3



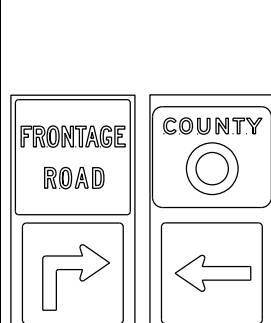
J4-1



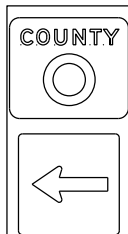
J4-2



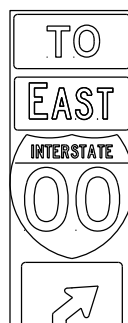
J4-2



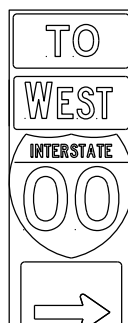
J12-1



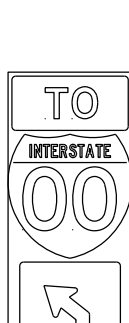
J13-1



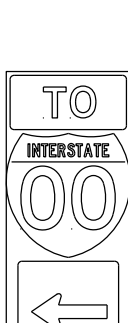
J32-1



J33-1



J22-1



J23-1



JR13-1



JR23-1

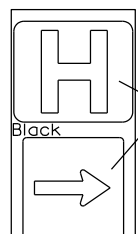


JR99-1



JV

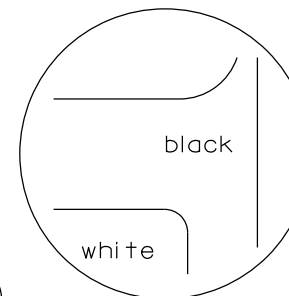
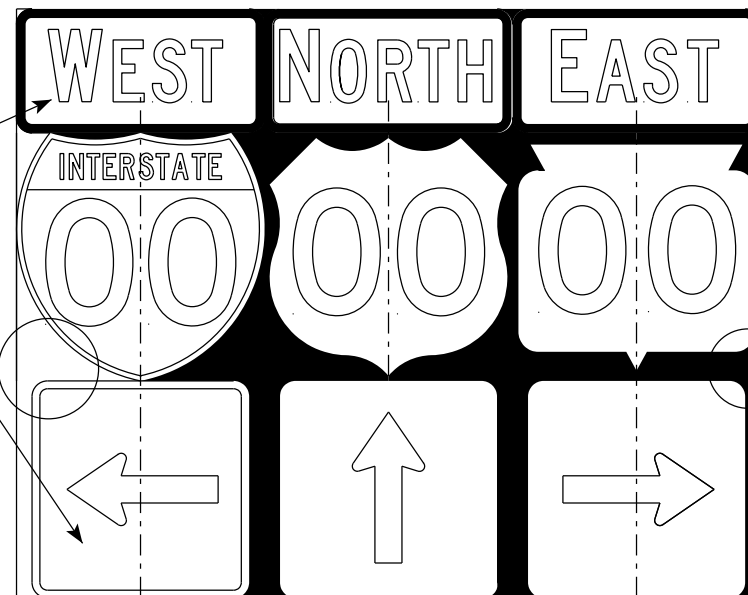
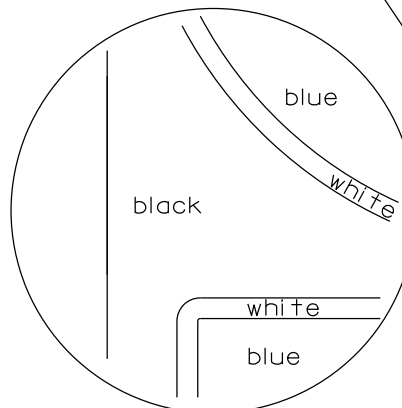
(Typical Vertical J-Assembly See Note 10 and 11)



JH-1

Blue Background

blue background with interstate



black background

## NOTES

- Signs are Type II - Type H Reflective
- Color:
  - Background - Black Non-reflective
  - Message - see Note 5
- Message Series - See Note 5
- Corners shall be square or rounded if base material is plywood. If base material is metal the corners shall be rounded.
- The colors and message spacing on each marker shall be according to the applicable route marker panel specifications.
- Certain marker heads require the component pieces to be the same color. As an example, all the components used with an MI-1 Interstate marker shall be blue.
- Single panel j-assemblies shall only be used with route marker shields that are same size. If the route marker shields are different size use multiple piece component.
- Route assemblies that have 24 inch route shields and have dimensions greater than 48 inches (both vertical and horizontal) shall have one horizontal splice between the arrows and route shields. Vertical splices shall not be used on route assemblies with a horizontal dimension of 144 inches or less. The contractor shall not use more than one vertical joint per sign and the joint shall be between route shields.
- Route assemblies that have 36 inch shields and have dimensions greater than 48 inches (both vertical and horizontal) shall have two horizontal splices. One horizontal splice shall be between the cardinal direction and route shields and the other horizontal splice shall be between the arrows and route shields. Vertical splices shall not be used on route assemblies with a horizontal dimension of 144 or less. The contractor shall not use more than one vertical joint per sign and the joint shall be between route shields.
- All Vertical J Assemblies are given a Sign Code of JV
- For JV Assemblies that have a mixture of Interstate and non Interstate shields, arrows and cardinals shall be white on blue.

### ROUTE MARKERS & COMPONENTS IN TYPICAL ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

*Matthew R. Rauch*  
for State Traffic Engineer

DATE 3/18/21

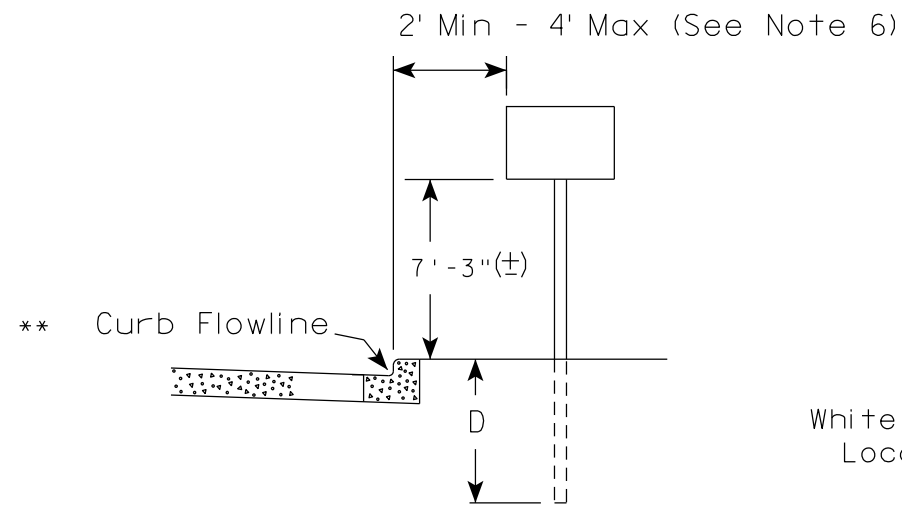
PLATE NO. A2-1S.9

PROJECT NO:

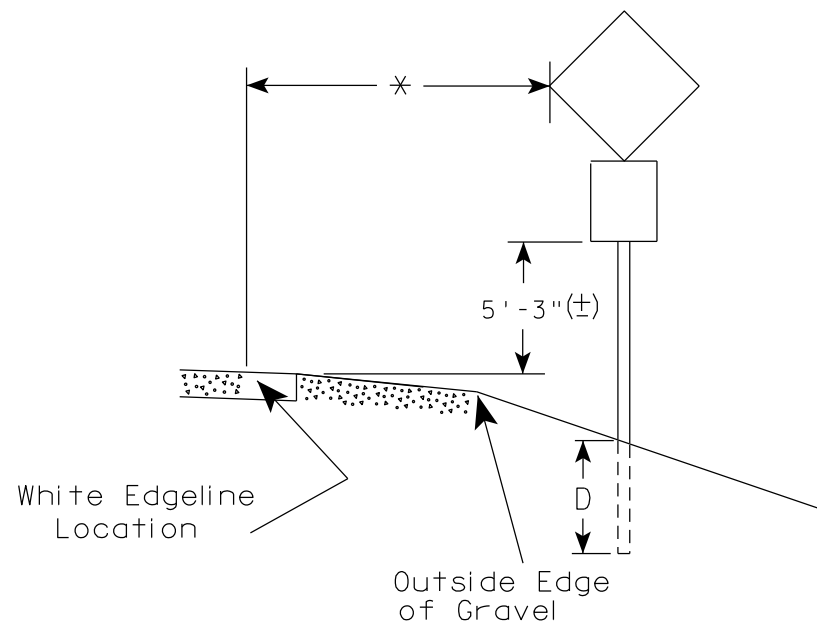
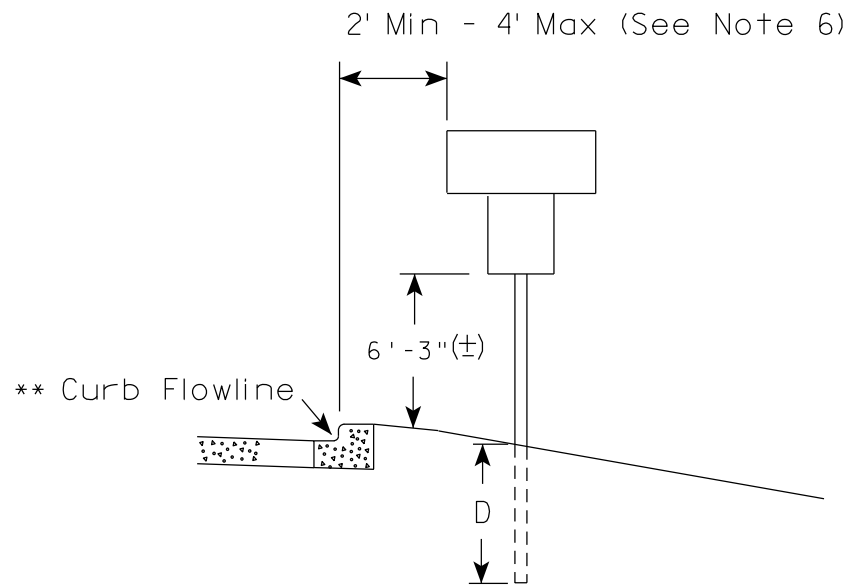
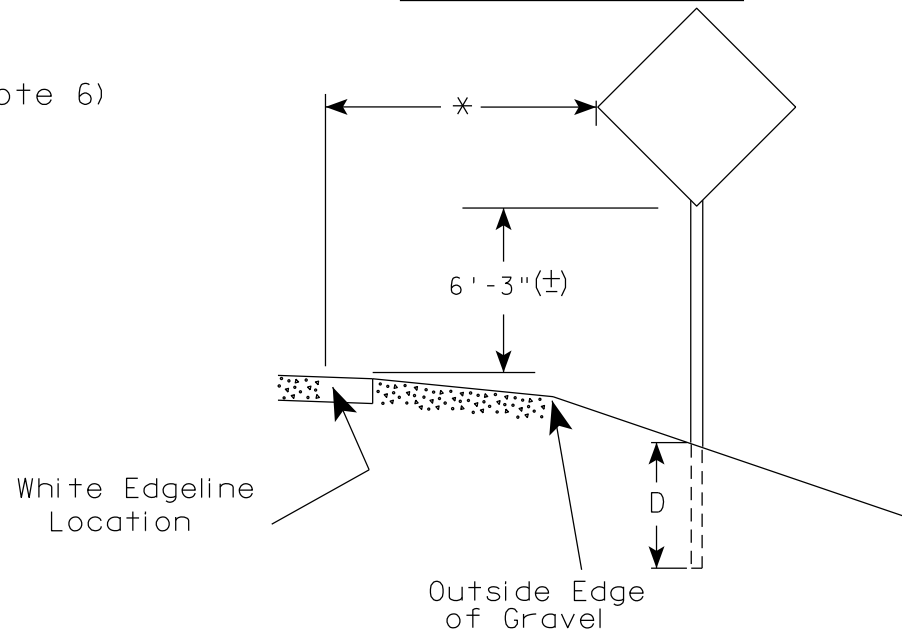
SHEET NO:

E

URBAN AREA



RURAL AREA (See Note 2)



POST EMBEDMENT DEPTH

Area of Sign Installation ( Sq. Ft. )	D ( Min )
20 or Less	4'
Greater than 20	5'

- GENERAL NOTES**
1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
  2. If signs are mounted on or behind barrier wall, see A4-10 sign plate.  
The Double Arrow sign (W12-1D) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).
  3. For expressways and freeways, mounting height is 7'- 3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
  4. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±).
  5. Offset distance shall be consistent with existing signs or consistent throughout length of project.
  6. The (±) tolerance for mounting height is 3 inches.
  7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the Engineer.

\* \* The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

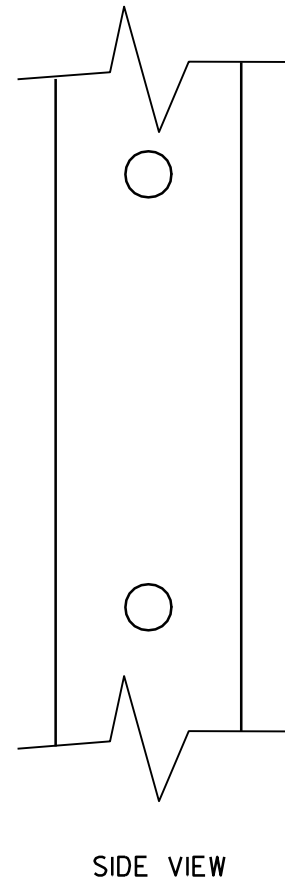
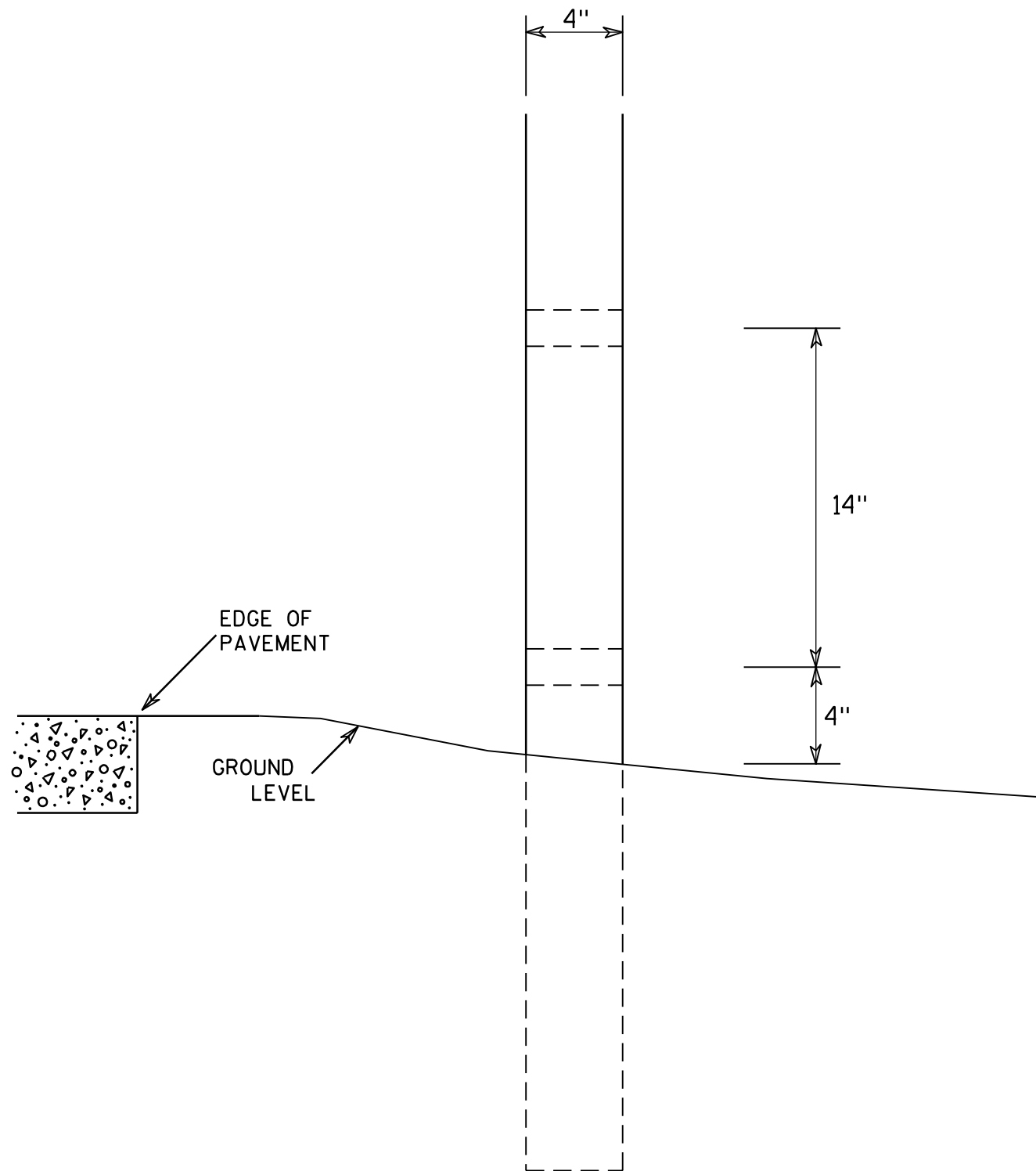
\* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 5/13/2020 PLATE NO. A4-3.22



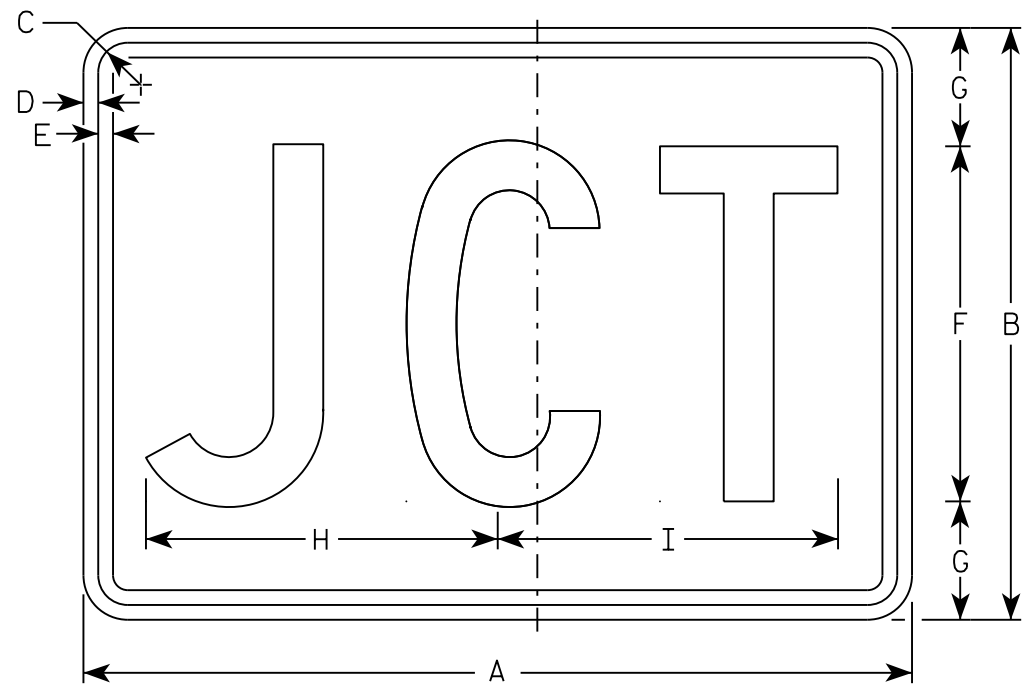
GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

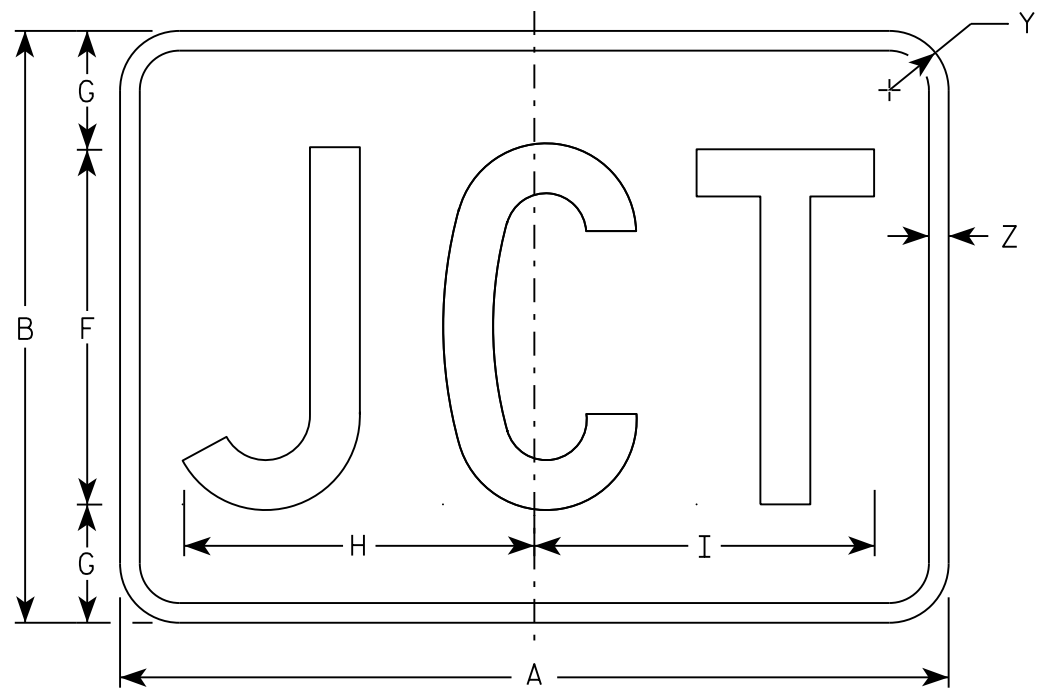
7

7

<b>4 X 6 WOOD POST MODIFICATIONS</b>	
<i>WISCONSIN DEPT OF TRANSPORTATION</i>	
APPROVED	<i>Chester J Spang</i> for State Traffic Engineer
DATE 3/27/97	PLATE NO. A4-11.2



M2-1  
MM2-1  
MP2-1



MB2-1  
MK2-1  
MN2-1  
MR2-1

NOTES

1. Sign is Type II - Type H
2. Color:  
Background - See note 5  
Message - See note 5
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. M2-1 Background - White  
Message - Black  
MB2-1 Background - Blue  
Message - White  
MK2-1 Background - Green  
Message - White  
MM2-1 Background - White  
Message - Green  
MN2-1 Background - Brown  
Message - White  
MP2-1 Background - White  
Message - Blue  
MR2-1 Background - Brown  
Message - Yellow

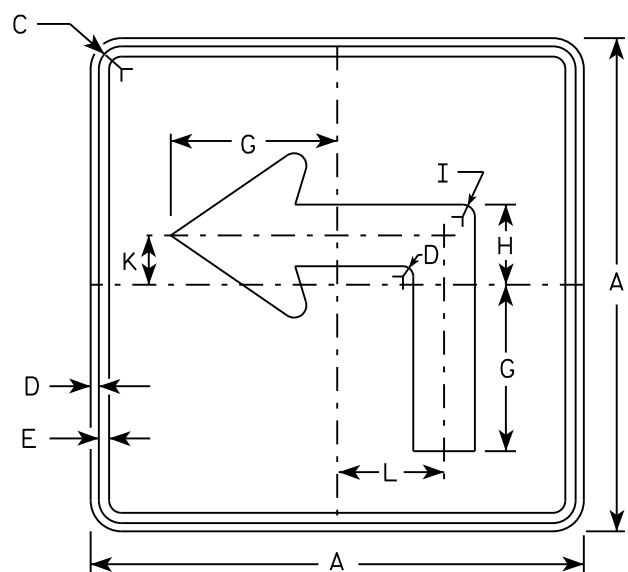
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21	15	1 1/8	3/8	3/8	9	3	8 7/8	8 5/8																1 1/2	1/2	2.20
3	30	21	1 1/8	3/8	3/8	13	4	12 7/8	12 3/8																1 1/2	1/2	4.40
4	30	21	1 1/8	3/8	3/8	13	4	12 7/8	12 3/8																1 1/2	1/2	4.40
5	30	21	1 1/8	3/8	3/8	13	4	12 7/8	12 3/8																1 1/2	1/2	4.40

STANDARD SIGN  
M2-1

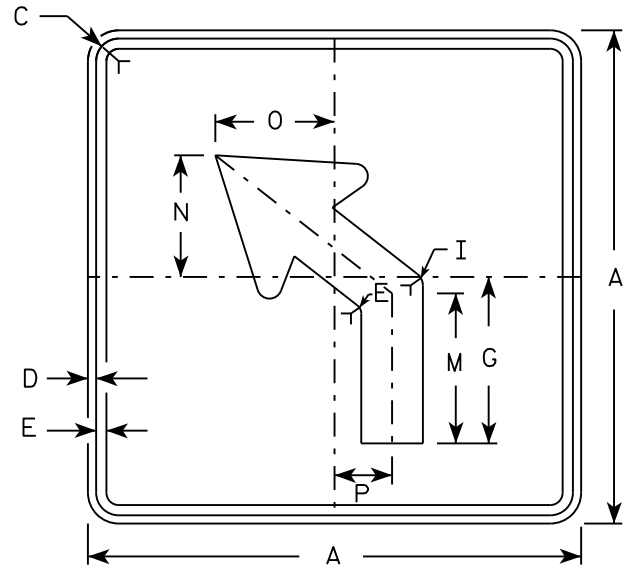
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
For State Traffic Engineer

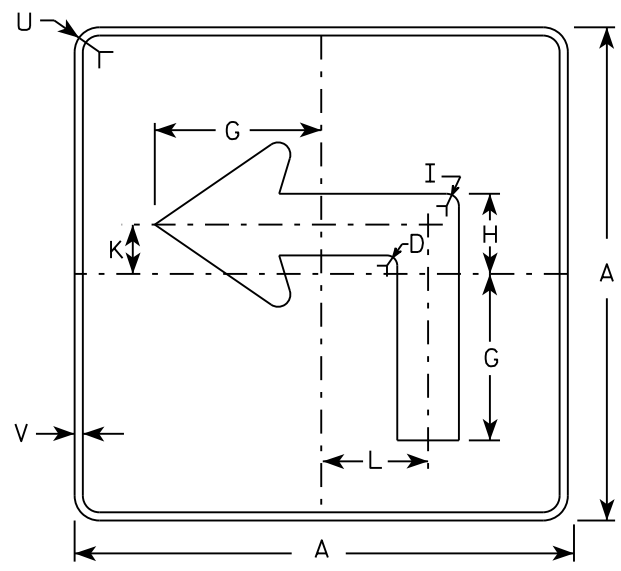
DATE 10/15/15 PLATE NO. M2-1.12



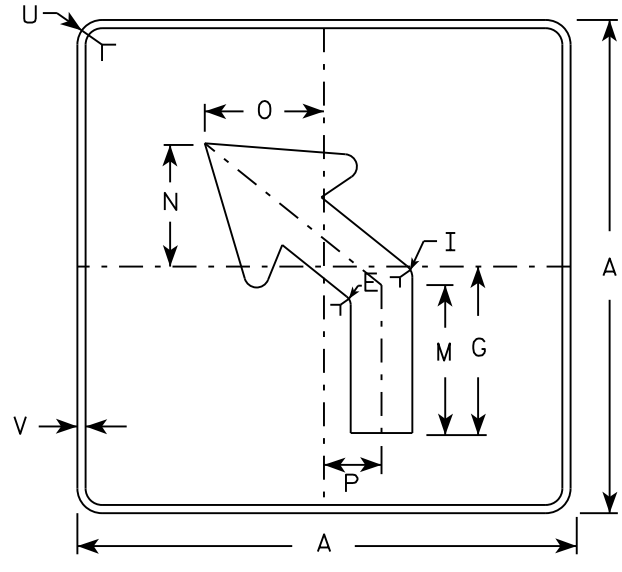
M5-1L  
MM5-1L  
M05-1L  
MP5-1L



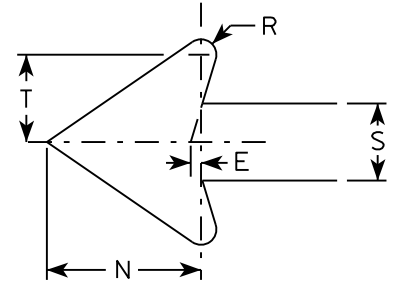
M5-2L  
MM5-2L  
M05-2L  
MP5-2L



MB5-1L  
MK5-1L  
MN5-1L  
MR5-1L



MB5-2L  
MK5-2L  
MN5-2L  
MR5-2L



NOTES

- Signs are Type II - Type H reflective except as shown
- Color:
  - Background - See note 4
  - Message - See note 4
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- |                 |                                         |
|-----------------|-----------------------------------------|
| M5-1 and M5-2   | Background - White                      |
|                 | Message - Black                         |
| MB5-1 and MB5-2 | Background - Blue                       |
|                 | Message - White                         |
| MK5-1 and MK5-2 | Background - Green                      |
|                 | Message - White                         |
| MM5-1 and MM5-2 | Background - White                      |
|                 | Message - Green                         |
| MN5-1 and MN5-2 | Background - Brown                      |
|                 | Message - White                         |
| M05-1 and M05-2 | Background - Orange - Type F Reflective |
|                 | Message - Black                         |
| MP5-1 and MP5-2 | Background - White - Type H Reflective  |
|                 | Message - Blue                          |
| MR5-1 and MR5-2 | Background - Brown                      |
|                 | Message - Yellow                        |
- M5-1R same as M5-1L except arrow points right.
- M5-2R same as M5-2L except arrow tilts right.

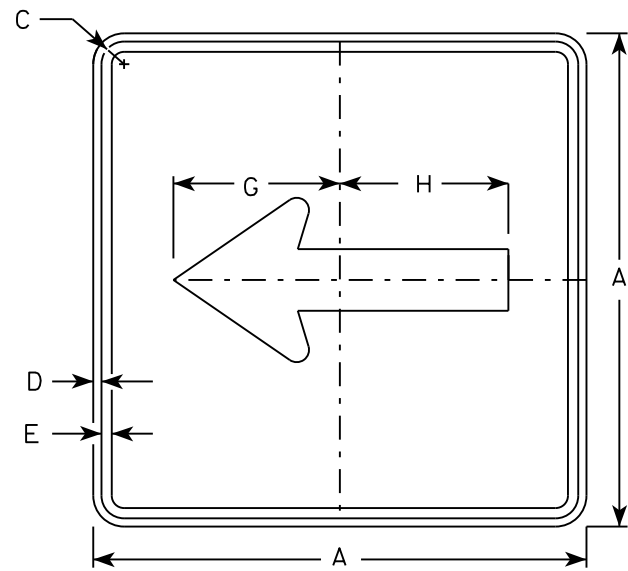
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7	3 3/8	5/8		2 1/8	4 1/2	6 3/8	5 1/4	5	2 1/2		1/2	2 5/8	3	1 1/2	1/2					3.06
3	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25
4	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25
5	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25

STANDARD SIGN  
M5-1 & M5-2

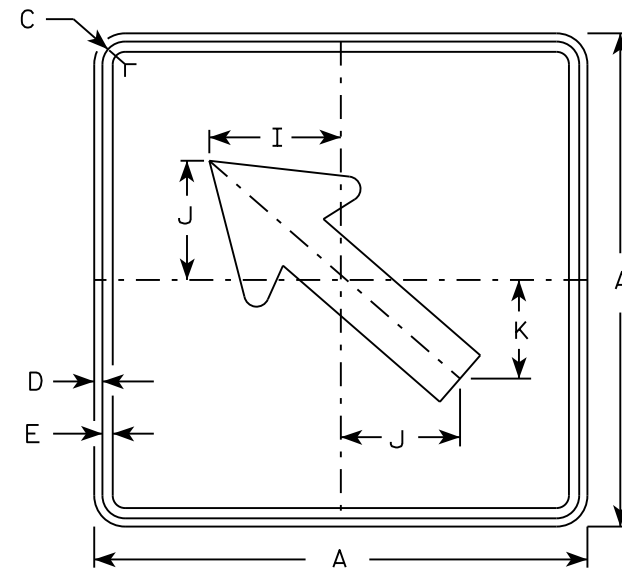
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

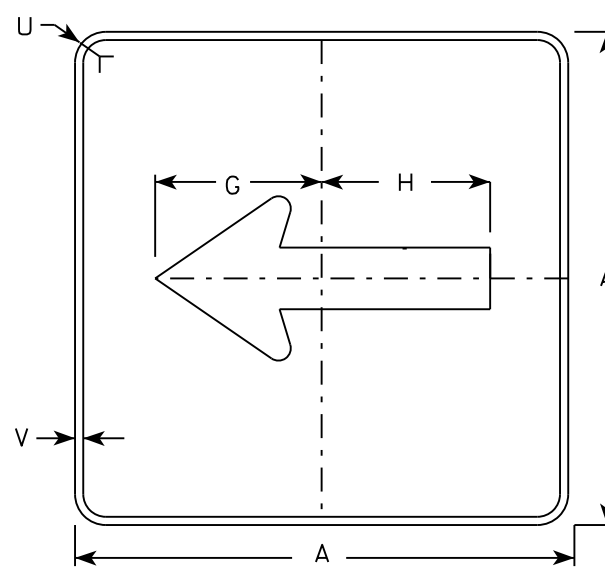
DATE 10/15/15 PLATE NO. M5-1.13



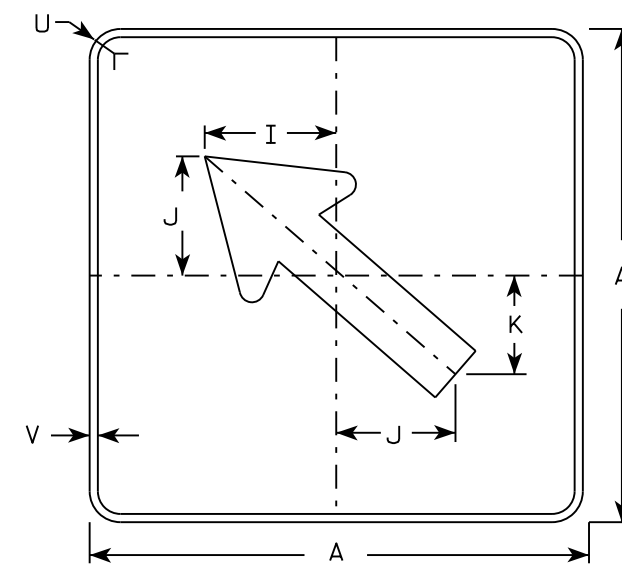
M6-1  
MM6-1  
M06-1  
MP6-1



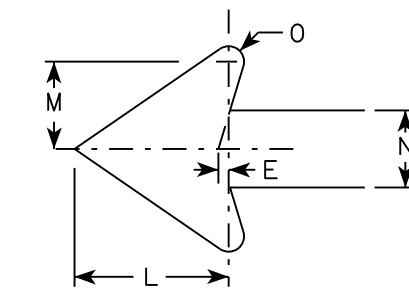
M6-2  
MM6-2  
M06-2  
MP6-2



MB6-1  
MK6-1  
MN6-1  
MR6-1



MB6-2  
MK6-2  
MN6-2  
MR6-2



NOTES

- Signs are Type II - Type H except as Shown
- Color:  
Background - See note 4  
Message - See note 4
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- M6-1 and M6-2 Background - White  
Message - Black  
MB6-1 and MB6-2 Background - Blue  
Message - White  
MK6-1 and MK6-2 Background - Green  
Message - White  
MM6-1 and MM6-2 Background - White  
Message - Green  
MN6-1 and MN6-2 Background - Brown  
Message - White  
M06-1 and M06-2 Background - Orange - Type F Reflective  
Message - Black  
MP6-1 and MP6-2 Background - White  
Message - Blue  
MR6-1 and MR6-2 Background - Brown  
Message - Yellow

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7 1/2	7 1/8	5 5/8	5	4 1/4	5 1/4	3	2 5/8	1/2						1 1/2	1/2					3.06
3	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25
4	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25
5	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25

STANDARD SIGN  
M6-1 & M6-2  
SERIES

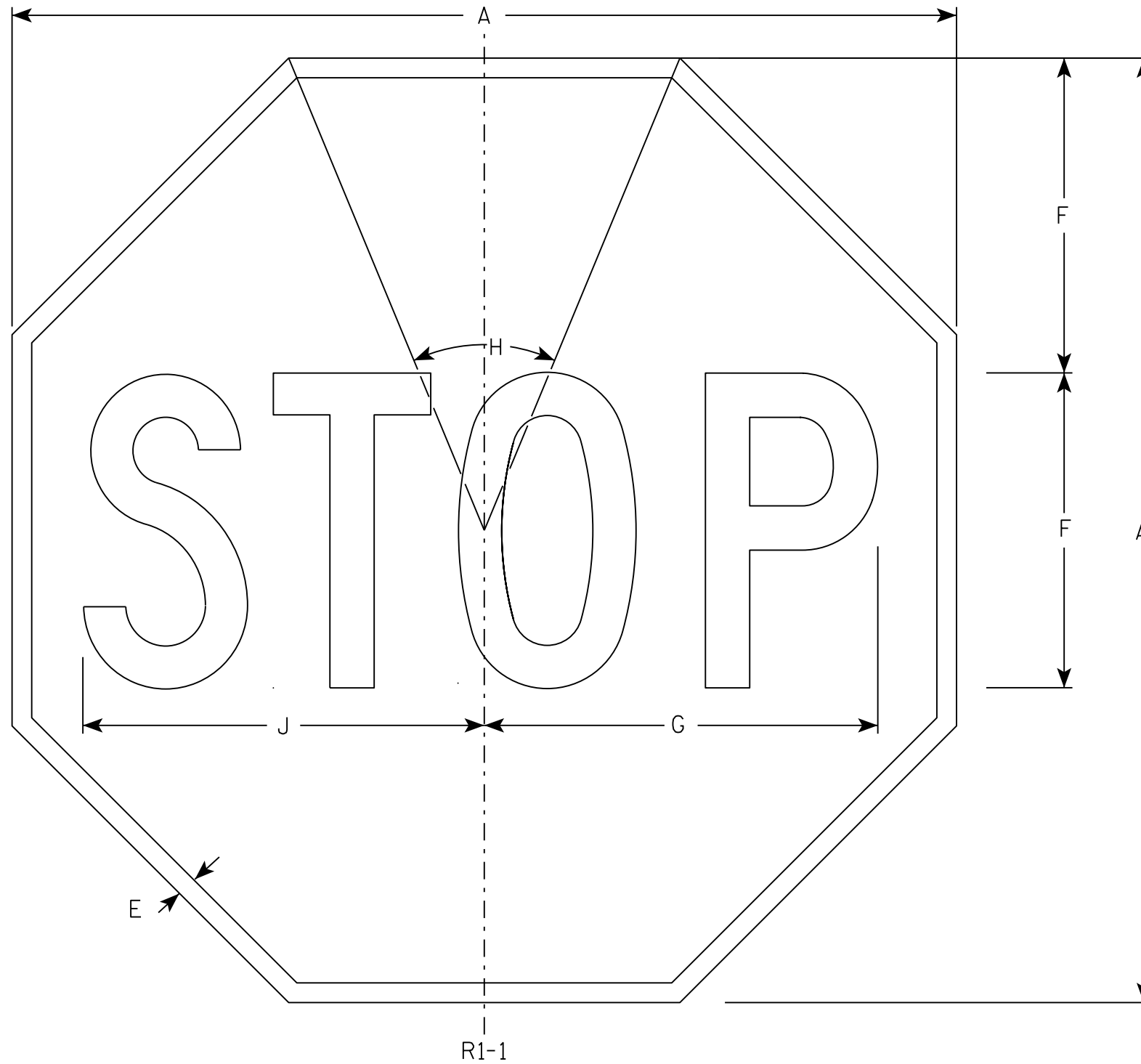
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 10/15/15 PLATE NO. M6-1.15

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - Red  
Message - White
3. Message Series - C



R1-1

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30				5/8	10	12 1/2	45°		12 3/4																	5.18
2S	30				5/8	10	12 1/2	45°		12 3/4																	5.18
2M	36				3/4	12	15	45°		15 3/8																	7.46
3	36				3/4	12	15	45°		15 3/8																	7.46
4	48				1	16	20	45°		20 1/2																	13.25
5	48				1	16	20	45°		20 1/2																	13.25
6	18				3/8	6	7 3/4	45°		7 3/4																	1.86
7	12				1/4	4	5	45°		5 1/8																	0.78

STANDARD SIGN  
R1-1

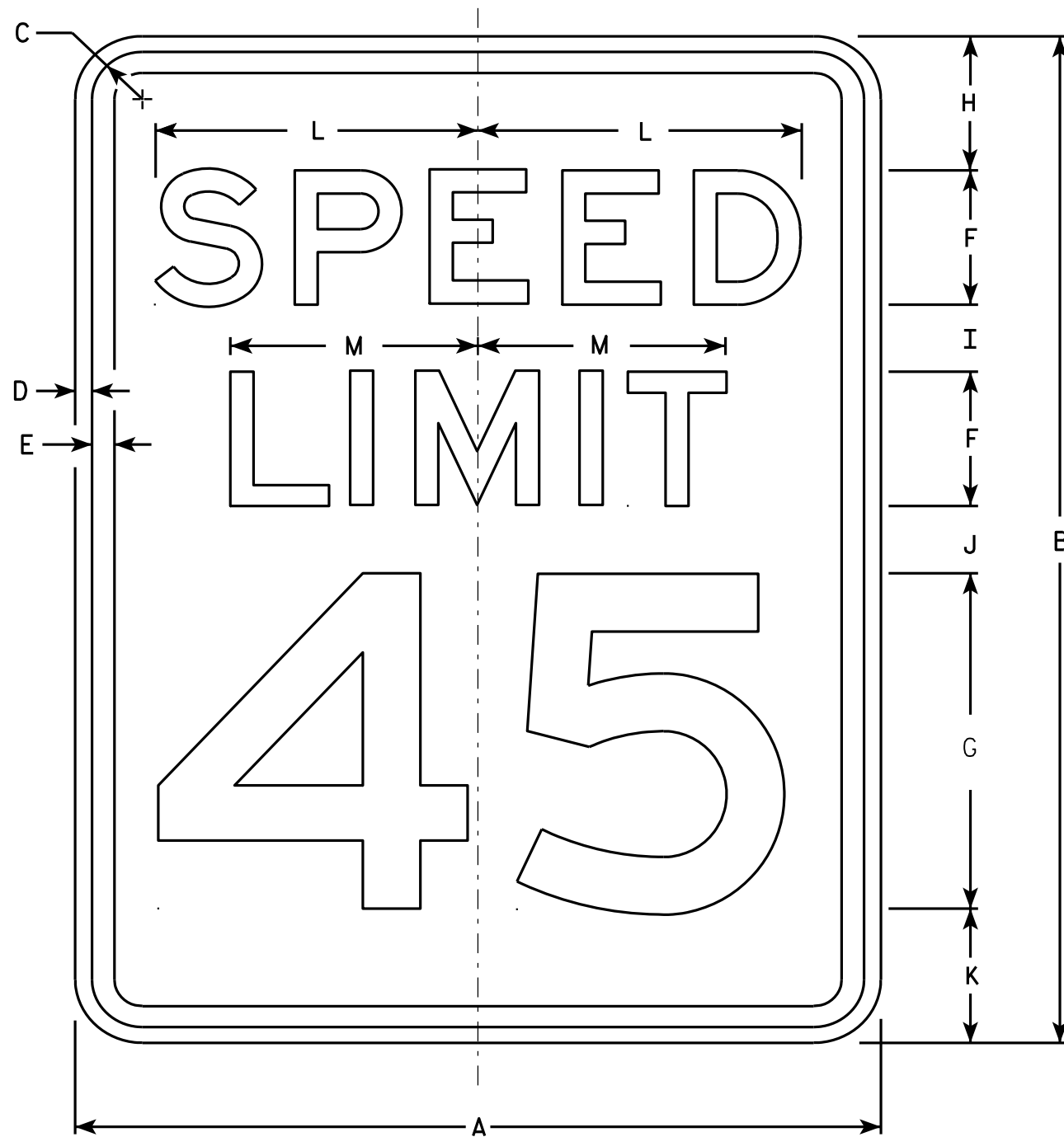
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 11/12/15 PLATE NO. R1-1.13

PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: **E**





R2-1

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - White  
Message - Black
3. Message Series - E
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals and optically adjust spacing to achieve proper balance.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18	24	1 1/8	3/8	1/2	3	8	3	2	2	3	7 1/4	5 1/2														3.0
2S	24	30	1 1/8	3/8	1/2	4	10	3	2 1/4	3 3/8	3 3/8	9 5/8	7 3/8														5.0
2M	30	36	1 3/8	1/2	5/8	5	12	5	2 1/2	2 1/2	4	12	9 1/4														7.5
3	36	48	1 3/8	1/2	5/8	6	14	6	5	5	6	14 3/8	11														12.0
4	36	48	1 3/8	1/2	5/8	6	14	6	5	5	6	14 3/8	11														12.0
5	48	60	2 1/4	3/4	1	8	20	6	4 1/2	6 3/4	6 3/4	19 1/4	14 5/8														20.0

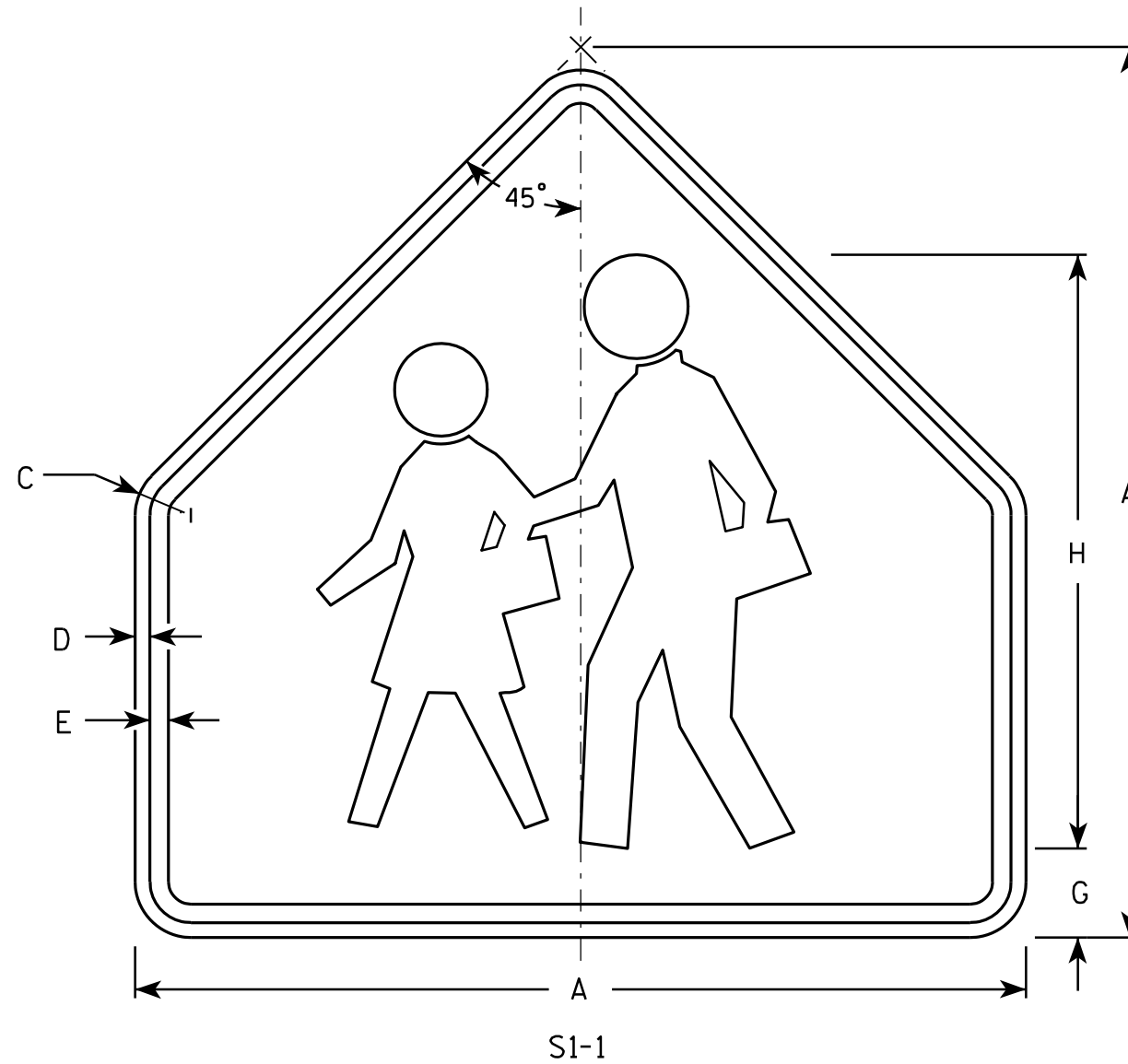
STANDARD SIGN  
R2-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
For State Traffic Engineer

DATE 5/26/10 PLATE NO. R2-1.13

PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: \_\_\_\_\_ E



**NOTES**

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - Yellow-Green  
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30		1 3/8	1/2	5/8		3	20																			4.69
2	36		1 5/8	5/8	3/4		3 1/2	24																			6.75
3	36		1 5/8	5/8	3/4		3 1/2	24																			6.75
4	48		2 1/4	3/4	1		4 3/4	32																			12
5																											

**STANDARD SIGN**  
S1-1

WISCONSIN DEPT OF TRANSPORTATION

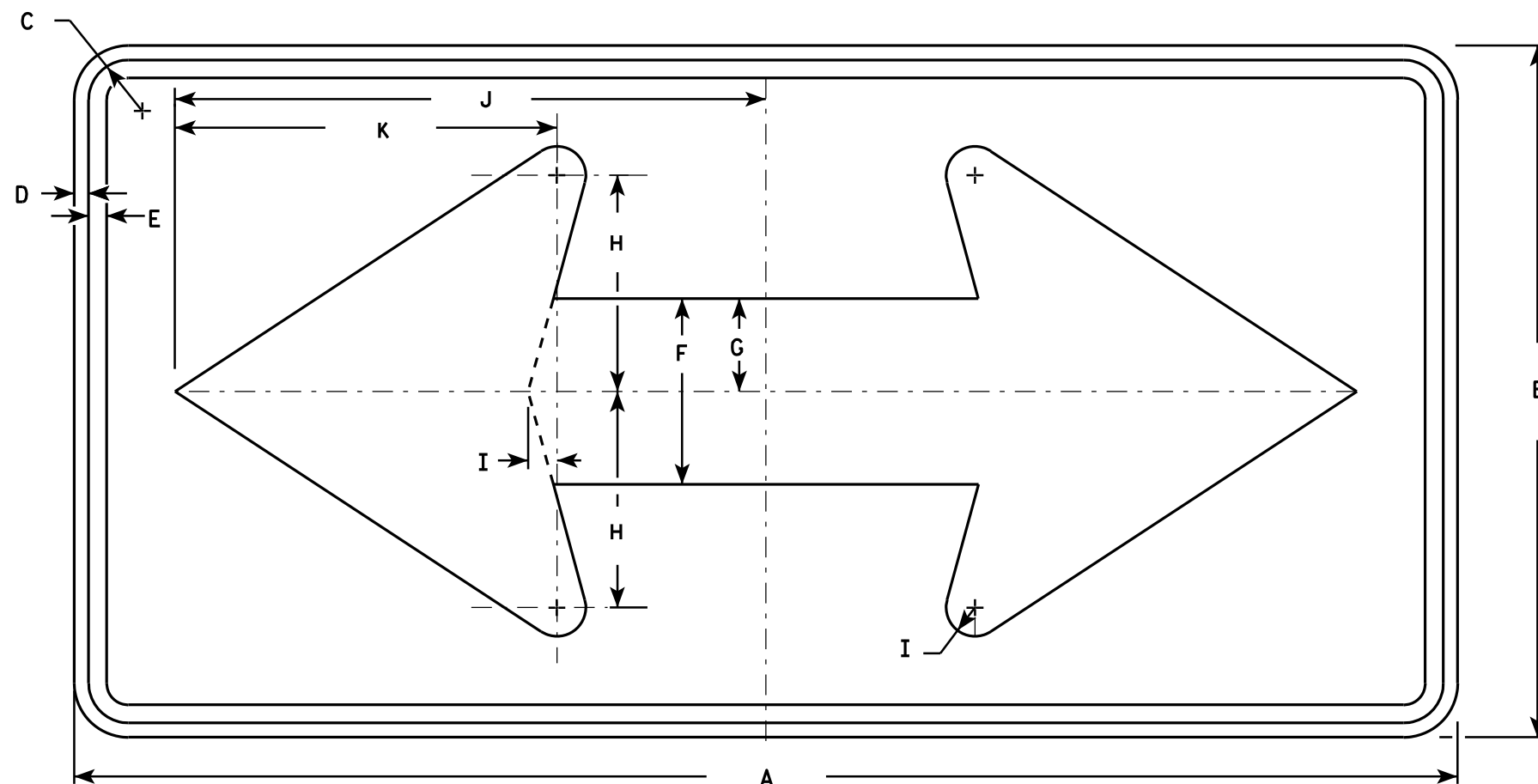
APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 6/30/05 PLATE NO. S1-1.8

PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: \_\_\_\_\_ E

**NOTES**

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - Yellow  
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



W1-7

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36	18	1 1/8	3/8	1/2	5	2 1/2	5 3/4	3/4	15 5/8	10 1/8																4.5
2S	48	24	1 3/8	1/2	5/8	6 1/2	3 1/4	7 1/2	1	20 1/2	13 1/4																8.0
2M	48	24	1 3/8	1/2	5/8	6 1/2	3 1/4	7 1/2	1	20 1/2	13 1/4																8.0
3	60	30	1 3/8	1/2	5/8	8	4	9 1/4	1 1/4	25 3/8	16 1/4																12.5
4	60	30	1 3/8	1/2	5/8	8	4	9 1/4	1 1/4	25 3/8	16 1/4																12.5
5	96	48	2 1/4	3/4	1	13	6 1/2	15	2	41	26 1/2																32.0

**STANDARD SIGN**  
W1-7

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
For State Traffic Engineer

DATE 6/7/10 PLATE NO. W1-7.7

PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: \_\_\_\_\_ E

NOTES

1. Sign is Type II - Type F Reflective
2. Color:  
Background - Yellow  
Message - Black
3. Message Series - C



W16-9P

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	12	1 1/8	3/8	3/8	5	3 1/2	3 1/8	17 3/4																		2.0
2M	30	18	1 1/8	3/8	1/2	7	5 1/2	2 3/4	24 1/2																		3.75
3	30	18	1 1/8	3/8	1/2	7	3 1/2	2 3/4	24 1/2																		3.75
4	48	24	1 3/8	1/2	5/8	10	7	6 1/8	35 3/4																		8.0
5																											

STANDARD SIGN  
W16-9P

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
State Traffic Engineer

DATE 3/7/19 PLATE NO. W16-9P.7

PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: **E**

7

7

CTH F 'E'											
CATEGORY	STATION	REAL STATION	DISTANCE	AREA (SF)		INCREMENTAL VOL (CY)		CUMULATIVE VOL (CY)			
				CUT	FILL	CUT (1)	FILL (2)	CUT 1.00 (1)	EXPANDED		MASS ORDINATE (4)
									FILL 1.25 (3)		
0010	21+00	2100	0	61.86	0.61	0	0	0	0	0	
	21+50	2150	50	62.09	19.94	115	19	115	24	91	
	21+85	2185	35	81.65	0.00	93	13	208	40	168	
	22+00	2200	15	69.78	1.14	42	0	250	40	210	
	22+50	2250	50	86.90	3.26	145	4	395	45	350	
	23+00	2300	50	105.00	2.52	178	5	573	52	521	
	23+50	2350	50	92.01	1.41	182	4	755	57	699	
	24+00	2400	50	97.43	3.16	175	4	931	62	869	
	24+50	2450	50	100.22	3.04	183	6	1,114	69	1,044	
	25+00	2500	50	105.74	3.27	191	6	1,304	76	1,228	
	25+11	2511	11	122.50	2.76	46	1	1,351	78	1,273	
	25+50	2550	39	99.10	0.97	160	3	1,511	81	1,429	
	26+00	2600	50	87.14	5.53	172	6	1,683	89	1,594	
	26+50	2650	50	78.02	2.60	153	8	1,836	98	1,738	
	27+00	2700	50	84.33	0.18	150	3	1,987	101	1,885	
	27+50	2750	50	95.91	0.77	167	1	2,153	103	2,051	
	28+00	2800	50	93.29	1.51	175	2	2,329	105	2,223	
	28+50	2850	50	82.82	5.94	163	7	2,492	114	2,378	
	29+00	2900	50	82.06	5.64	153	11	2,644	127	2,517	
	29+50	2950	50	104.19	2.03	172	7	2,817	136	2,681	
	30+00	3000	50	91.15	2.72	181	4	2,998	142	2,856	
	30+50	3050	50	88.72	13.83	167	15	3,164	161	3,003	
	31+00	3100	50	82.07	3.42	158	16	3,322	181	3,142	
	31+50	3150	50	88.29	0.89	158	4	3,480	186	3,294	
	32+00	3200	50	114.45	5.19	188	6	3,668	193	3,475	
	32+50	3250	50	120.99	0.97	218	6	3,886	200	3,686	
	33+00	3300	50	113.98	0.88	218	2	4,103	202	3,901	
	33+50	3350	50	102.50	0.02	200	1	4,304	203	4,101	
	34+00	3400	50	99.49	2.03	187	2	4,491	205	4,285	
	34+50	3450	50	91.95	7.87	177	9	4,668	217	4,451	
	35+00	3500	50	89.80	12.86	168	19	4,836	241	4,595	
	35+50	3550	50	88.83	14.78	165	26	5,002	273	4,729	
	36+00	3600	50	85.96	19.17	162	31	5,164	312	4,851	
	36+50	3650	50	75.42	6.98	149	24	5,313	342	4,971	
	37+00	3700	50	85.25	14.16	149	20	5,462	367	5,095	
	37+50	3750	50	91.10	7.74	163	20	5,625	392	5,233	
	38+00	3800	50	100.37	3.61	177	11	5,802	405	5,397	
	38+50	3850	50	101.74	2.58	187	6	5,990	413	5,577	
	39+00	3900	50	98.60	5.11	186	7	6,175	421	5,754	
	39+50	3950	50	84.55	0.46	170	5	6,345	428	5,917	
	40+00	4000	50	83.25	5.31	155	5	6,500	435	6,065	
	40+50	4050	50	90.73	24.28	161	27	6,661	469	6,192	
						6661	375				

NOTES:  
(1) SALVAGED/UNUSABLE PAVEMENT MATERIAL IS INCLUDED IN CUT  
(2) DOES NOT INCLUDE UNUSABLE PAVEMENT EXC VOLUME  
(3) EXPANDED FILL FACTOR 1.25  
(4) THE MASS ORDINATE + OR - QTY CALCULATED FOR THE STAGE. PLUS QUANTITY INDICATES AN EXCESS OF MATERIAL WITHIN THE STAGE. MINUS INDICATES A SHORTAGE OF MATERIAL WITHIN THE STAGE

9

9

CTH F E											
CATEGORY	STATION	REAL STATION	DISTANCE	AREA (SF)		INCREMENTAL VOL (CY)		CUMULATIVE VOL (CY)			
				CUT	FILL	CUT (1)	FILL (2)	CUT 1.00 (1)	EXPANDED		MASS ORDINATE (4)
									FILL 1.25 (3)		
0010	40+50	4050	50	90.73	24.28	162	30	6,661	469	6,192	
	41+00	4100	50	83.26	24.74	161	45	6,822	526	6,297	
	41+50	4150	50	84.95	22.17	156	43	6,978	580	6,398	
	41+68	4168	18	105.97	7.90	64	10	7,042	592	6,449	
	42+00	4200	32	92.71	2.04	118	6	7,159	600	6,560	
	42+17	4217	17	124.17	0.15	68	1	7,228	601	6,627	
	42+50	4250	33	114.96	0.00	146	0	7,374	601	6,773	
	43+00	4300	50	133.03	2.80	230	3	7,603	604	6,999	
	43+18	4318	18	129.44	0.01	87	1	7,691	605	7,086	
	43+50	4350	32	132.64	3.19	155	2	7,846	608	7,239	
	43+67	4367	17	120.54	0.05	80	1	7,926	609	7,317	
	44+00	4400	33	98.97	2.91	134	2	8,060	611	7,449	
	44+50	4450	50	97.50	6.11	182	8	8,242	622	7,620	
	45+00	4500	50	100.58	6.87	183	12	8,425	637	7,789	
	45+27	4527	27	120.14	4.82	110	6	8,536	644	7,892	
	45+50	4550	23	104.64	1.46	96	3	8,631	647	7,984	
	45+61	4561	11	121.97	0.23	46	0	8,678	648	8,030	
	45+89	4589	28	134.81	0.33	133	0	8,811	648	8,163	
	46+00	4600	11	113.30	0.54	51	0	8,861	648	8,213	
	46+50	4650	50	120.89	1.05	217	1	9,078	650	8,428	
	47+00	4700	50	176.74	0.00	229	1	9,307	651	8,656	
	47+24	4724	24	131.57	0.00	115	0	9,422	651	8,771	
	47+50	4750	26	99.24	0.14	111	0	9,533	651	8,882	
	48+00	4800	50	83.56	1.86	169	2	9,703	654	9,049	
	48+50	4850	50	85.98	11.75	157	13	9,860	669	9,190	
	49+00	4900	50	93.36	4.84	166	15	10,026	689	9,337	
	49+50	4950	50	88.75	9.24	169	13	10,194	705	9,489	
	50+00	5000	50	119.27	4.40	193	13	10,387	721	9,666	
	50+50	5050	50	109.99	3.17	212	7	10,599	729	9,870	
	50+67	5067	17	111.81	3.48	70	2	10,669	732	9,937	
	51+00	5100	33	90.49	4.87	124	5	10,793	738	10,054	
	51+50	5150	50	81.97	6.31	160	10	10,952	751	10,201	
	52+00	5200	50	93.24	6.12	162	12	11,114	766	10,349	
	52+25	5225	25	83.04	2.37	82	4	11,196	771	10,425	
	52+50	5250	25	67.98	16.76	70	9	11,266	782	10,484	
	52+72	5272	22	76.80	9.29	59	11	11,325	795	10,530	
	53+00	5300	28	77.44	14.15	80	12	11,405	810	10,595	
	53+50	5350	50	82.73	8.53	148	21	11,553	836	10,717	
	53+69	5369	19	75.38	6.25	56	5	11,609	843	10,766	
	54+00	5400	31	64.54	8.08	80	8	11,689	853	10,836	
	54+36	5436	36	87.48	2.01	101	7	11,791	862	10,929	
	54+50	5450	14	73.12	1.44	42	1	11,832	863	10,969	
							5171	315			

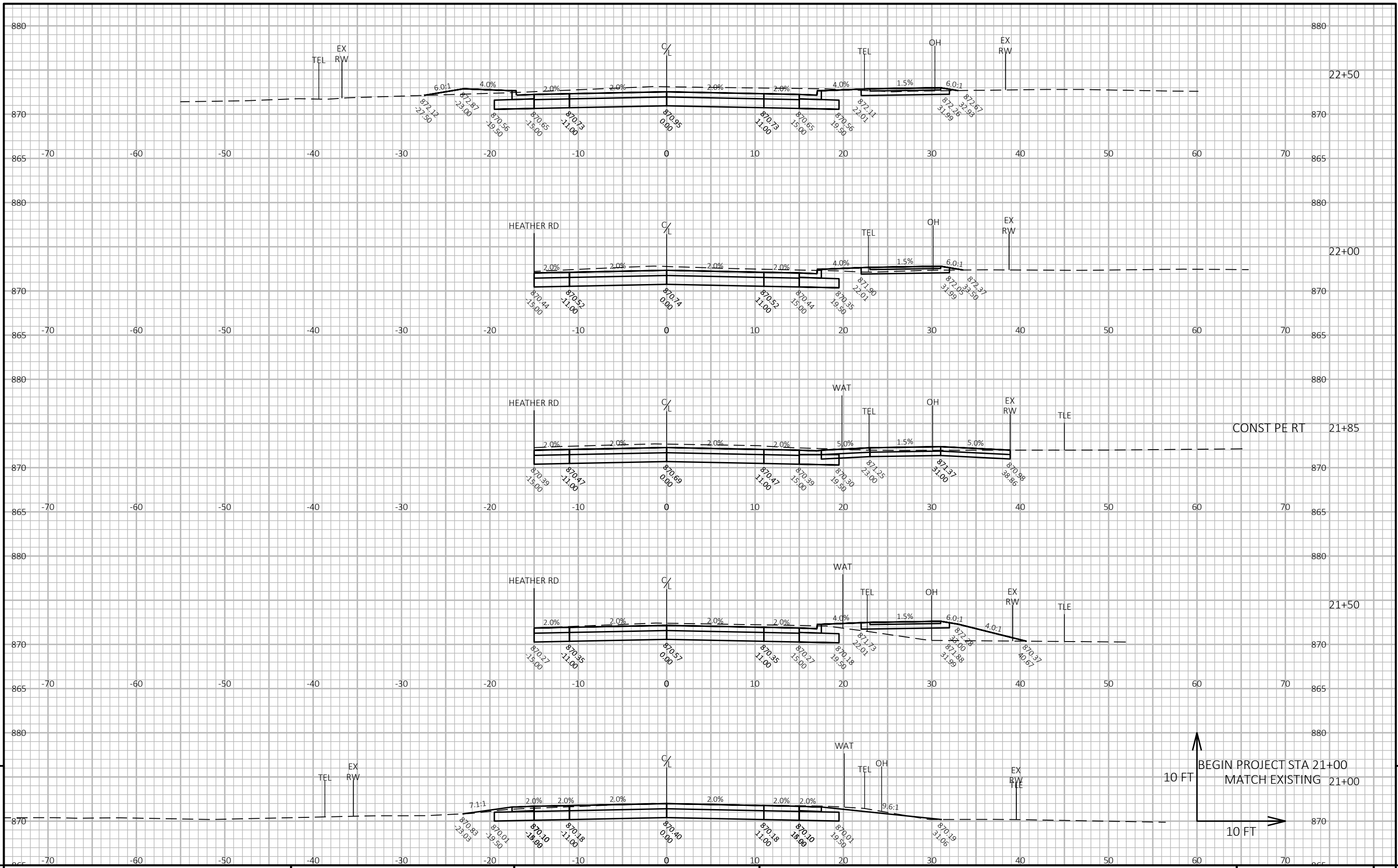
NOTES:  
(1) SALVAGED/UNUSABLE PAVEMENT MATERIAL IS INCLUDED IN CUT  
(2) DOES NOT INCLUDE UNUSABLE PAVEMENT EXC VOLUME  
(3) EXPANDED FILL FACTOR = 1.25  
(4) THE MASS ORDINATE + OR - QTY CALCULATED FOR THE STAGE. PLUS QUANTITY INDICATES AN EXCESS OF MATERIAL WITHIN THE STAGE. MINUS INDICATES A SHORTAGE OF MATERIAL WITHIN THE STAGE

9

9

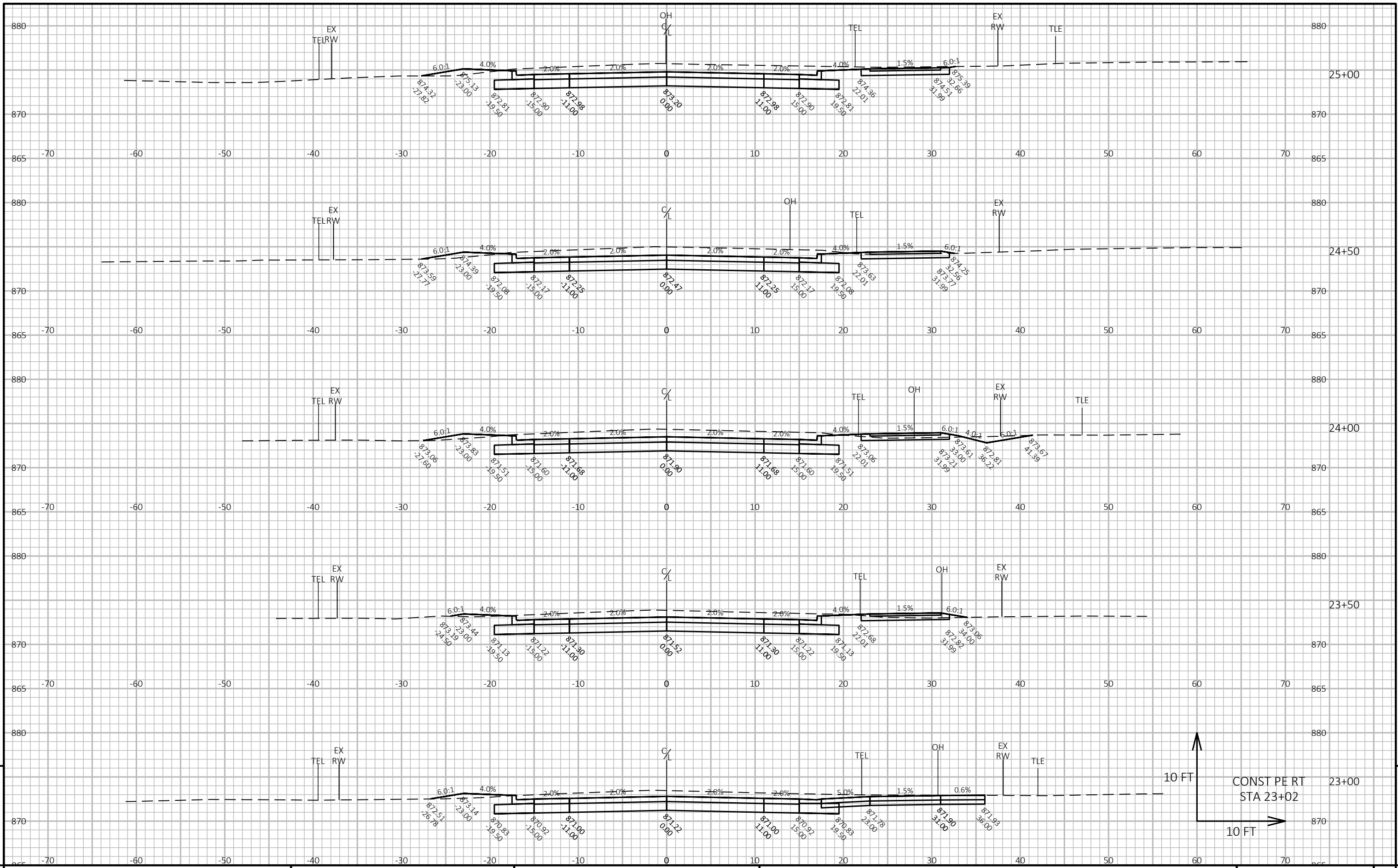
W. LOWES CREEK RD 'LC' - NORTH DITCH										
CATEGORY	STATION	REAL STATION	DISTANCE	AREA (SF)		INCREMENTAL VOL (CY)		CUMULATIVE VOL (CY)		
				CUT	FILL	CUT	FILL	CUT	EXPANDED FILL	MASS ORDINATE
						(1)	(2)	(1)	(3)	(4)
0010	3+00	300	0	0.00	0.00	0	0	0	0	0
	3+50	350	50	26.35	0.00	24	0	24	0	24
	4+00	400	50	61.76	0.00	82	0	82	0	82
	4+50	450	50	68.46	0.00	121	0	202	0	202
	5+00	500	50	59.31	0.00	118	0	320	0	320
	5+50	550	50	44.56	0.00	96	0	417	0	417
	6+00	600	50	41.24	0.00	79	0	496	0	496
	6+50	650	50	58.43	0.00	92	0	588	0	588
	7+00	700	50	69.51	0.00	118	0	707	0	707
						731	0			

NOTES:  
(1) SALVAGED/UNSUALE PAVEMENT MATERIAL IS INCLUDED IN CUT  
(2) DOES NOT INCLUDE UNUSABLE PAVEMENT EXC VOLUME  
(3) EXPANDED FILL FACTOR = 1.25  
(4) THE MASS ORDINATE + OR - QTY CALCULATED FOR THE STAGE. PLUS QUANTITY INDICATES AN EXCESS OF MATERIAL WITHIN THE STAGE. MINUS INDICATES A SHORTAGE OF MATERIAL WITHIN THE STAGE



PROJECT NO: 7995-02-63      HWY: CTH F      COUNTY: EAU CLAIRE      CROSS SECTIONS: CTH F      SHEET 9

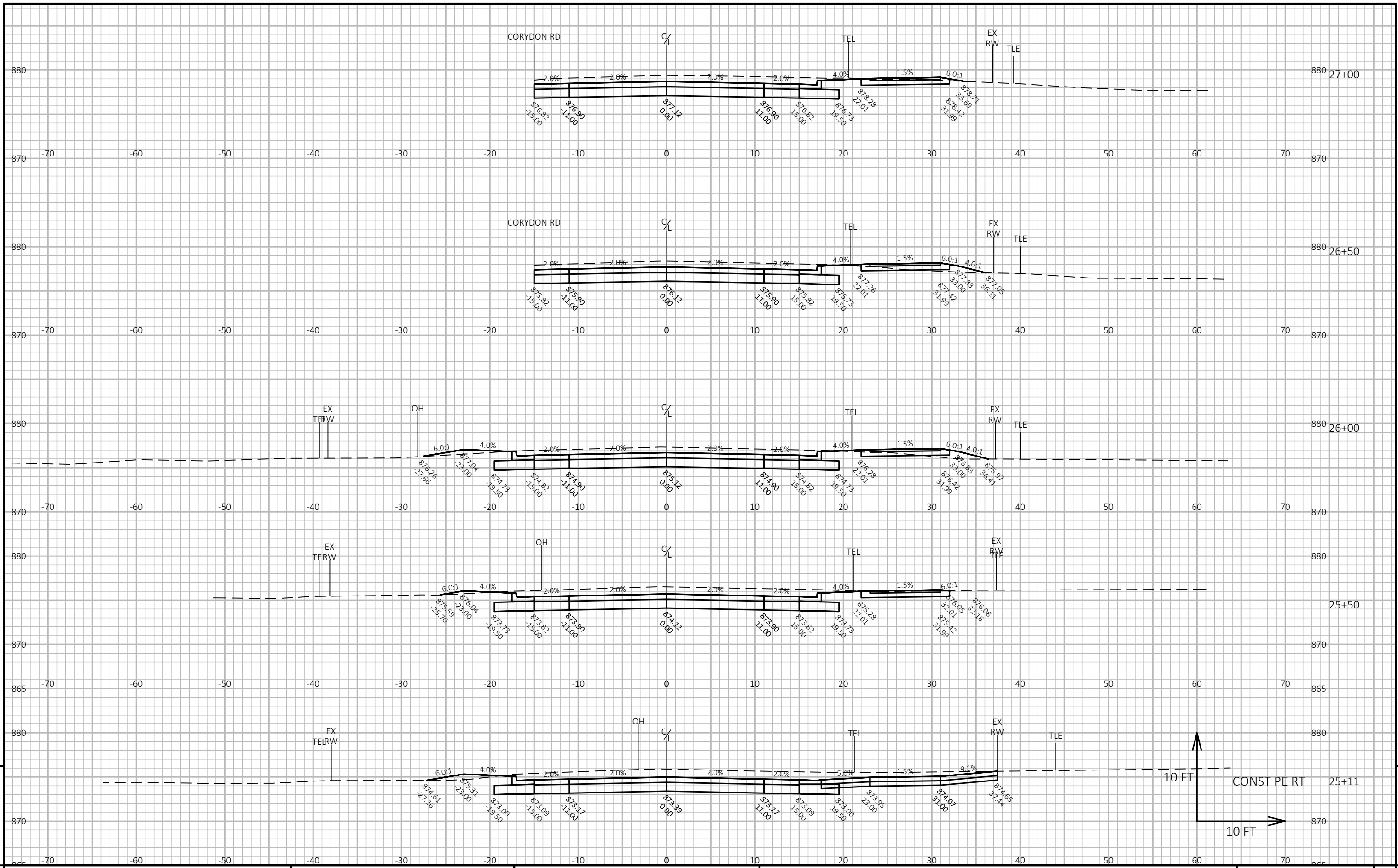




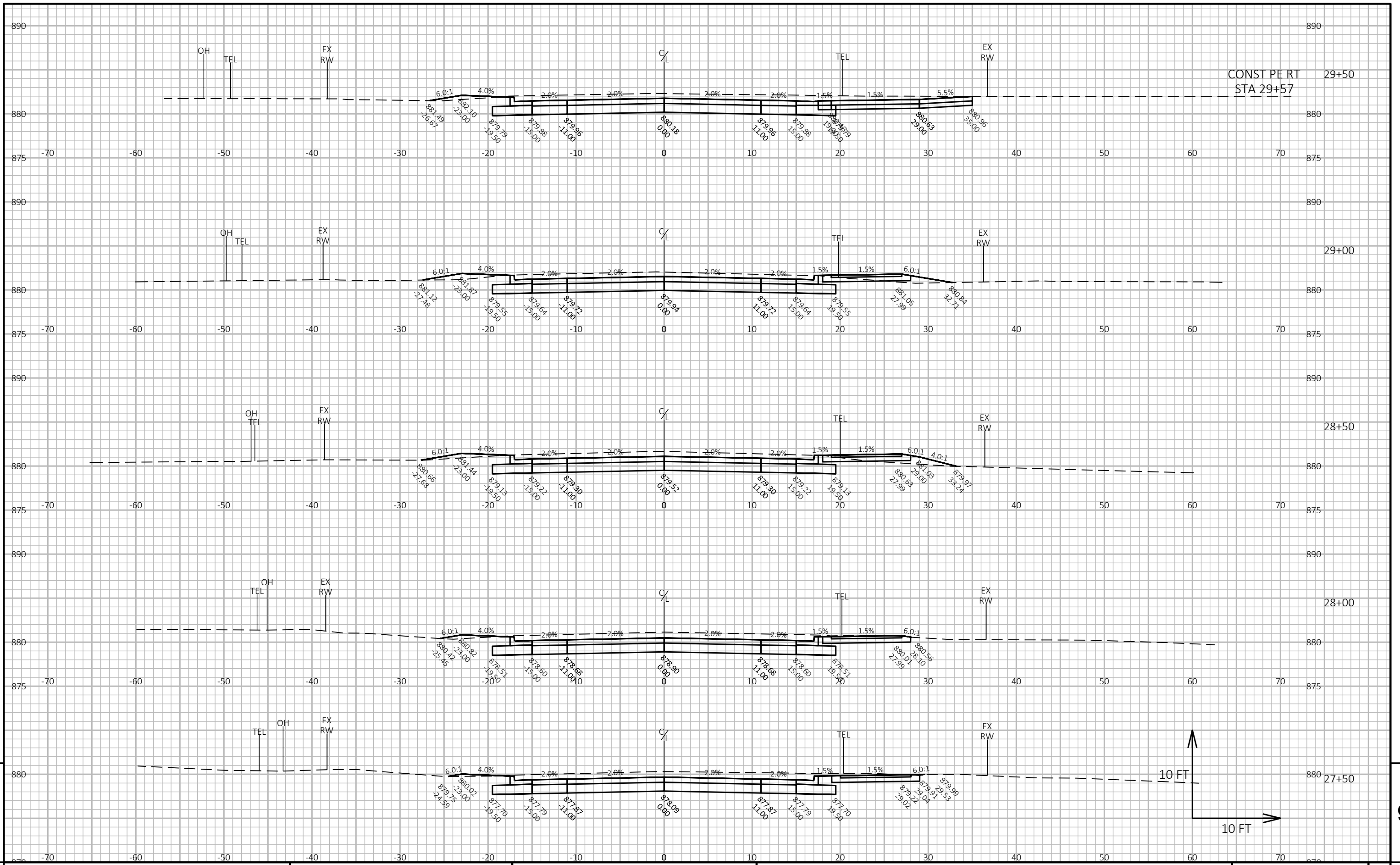
PROJECT NO: 7995-02-63      HWY: CTH F      COUNTY: EAU CLAIRE      CROSS SECTIONS: CTH F      SHEET      E

FILE NAME: \\SEHINC.COM\PANZURA\PROJECTS\AE\EAUCH\165167\5-FINAL-DSGN\CIVIL 3D-STATE ST\DESIGN\CORRIDORS\CRDR-CTH F.DWG      PLOT DATE: 1/18/2024 7:34 PM      PLOT BY: JUSTIN SHAVLIK      PLOT NAME:      PLOT SCALE: 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.      WISDOT/CADD SHEET 49

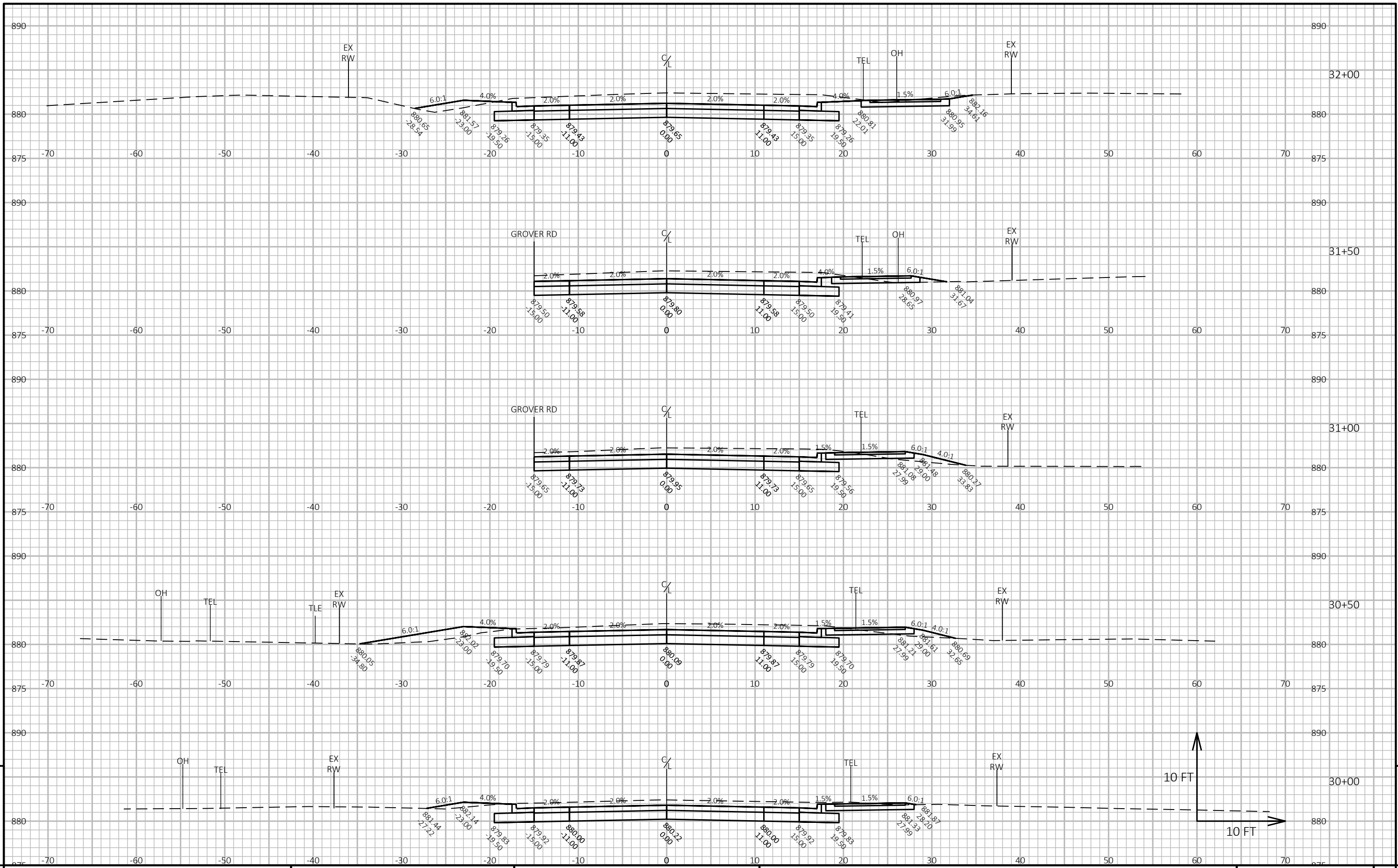
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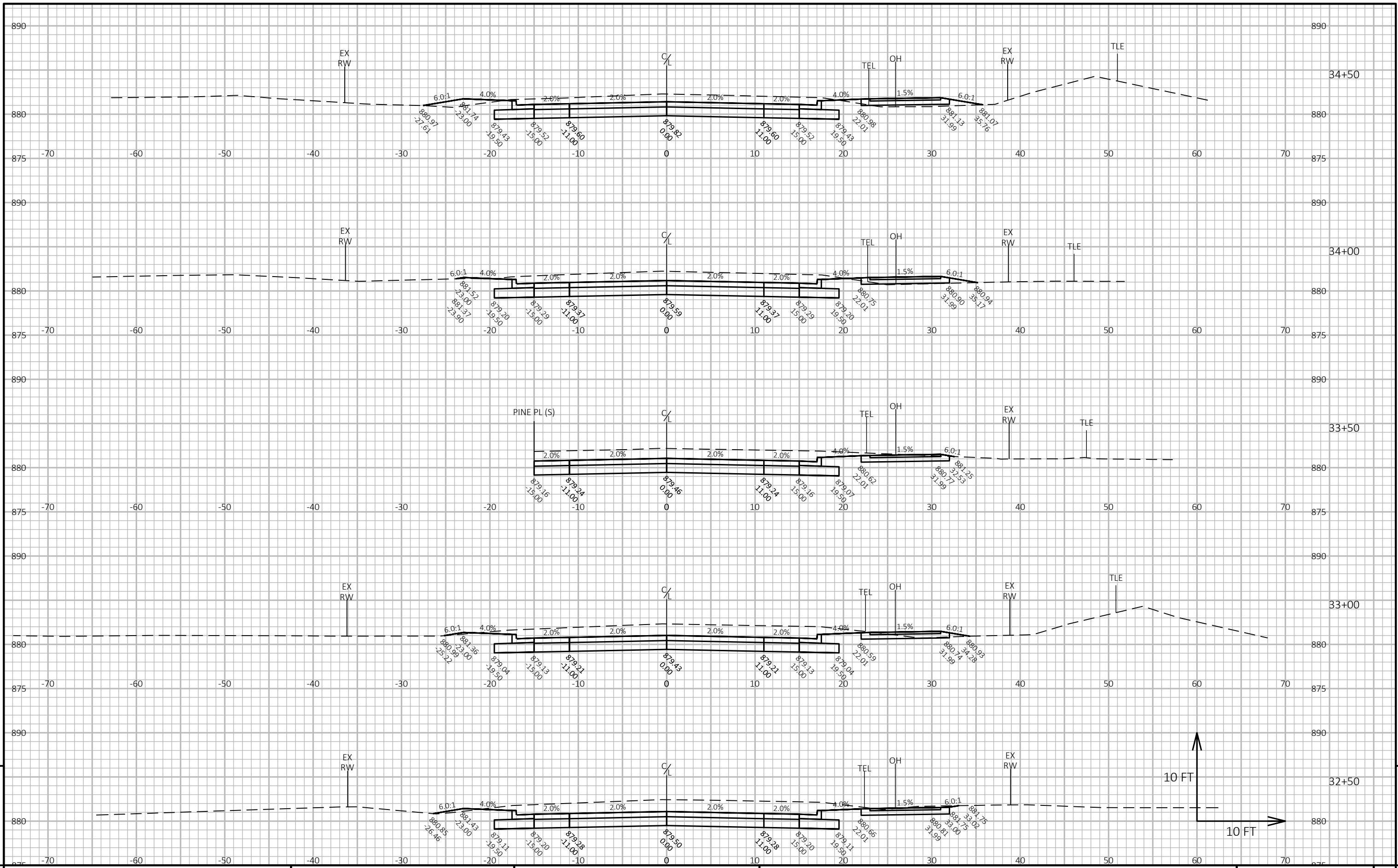
PROJECT NO: 7995-02-63      HWY: CTH F      COUNTY: EAU CLAIRE      CROSS SECTIONS: CTH F      SHEET      E



PROJECT NO: 7995-02-63      HWY: CTH F      COUNTY: EAU CLAIRE      CROSS SECTIONS: CTH F      SHEET      E

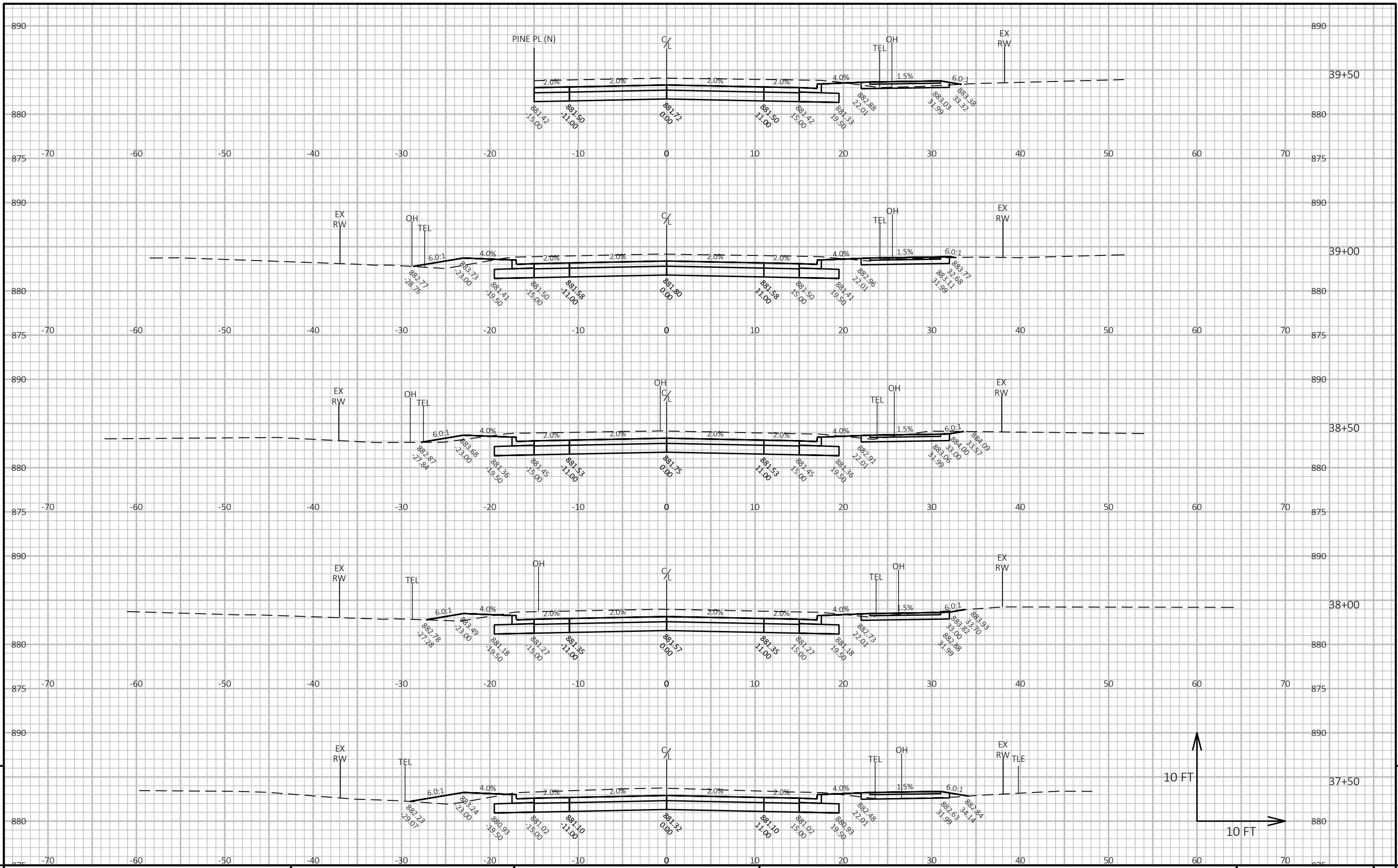


PROJECT NO: 7995-02-63      HWY: CTH F      COUNTY: EAU CLAIRE      CROSS SECTIONS: CTH F      SHEET      E

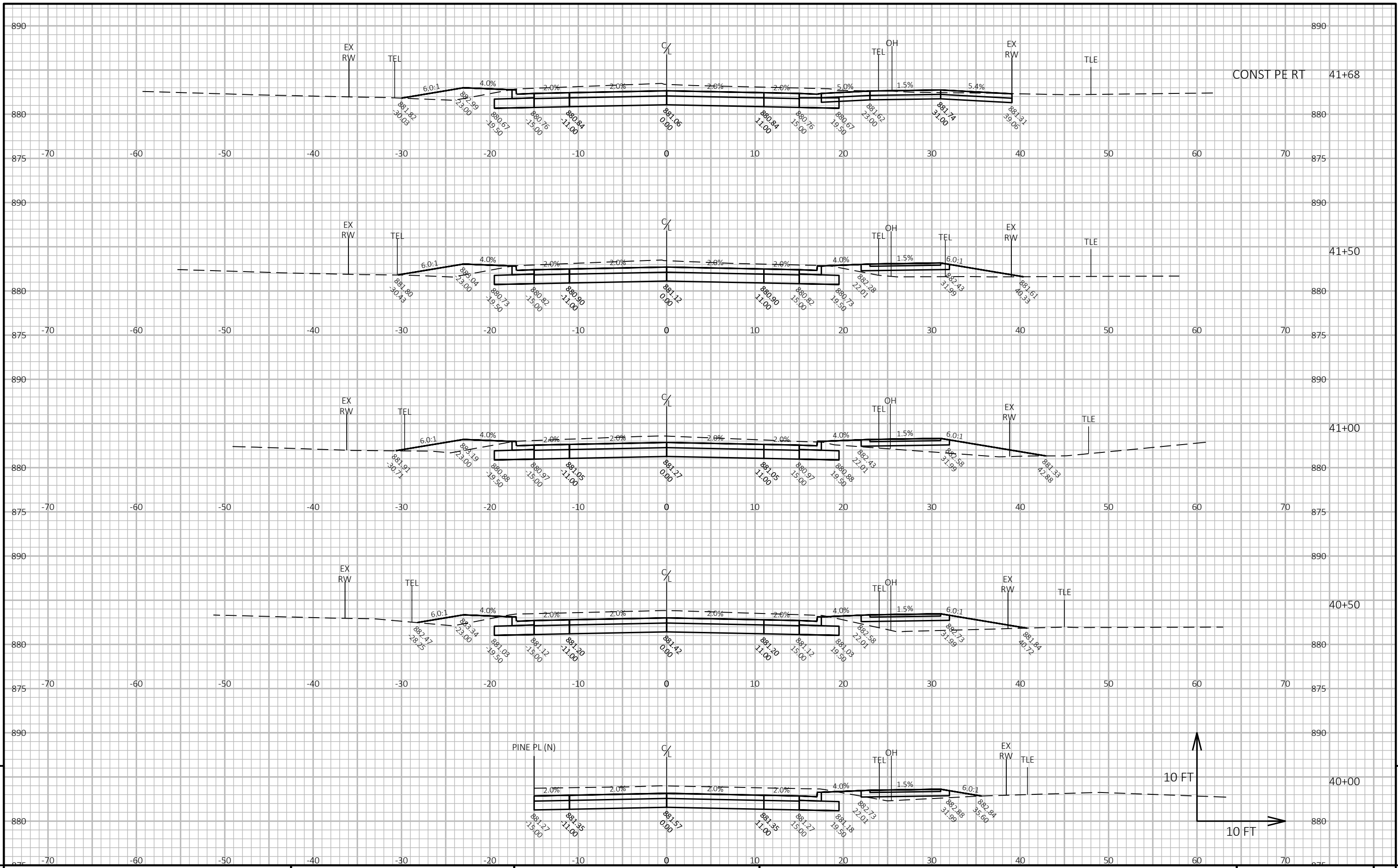


PROJECT NO: 7995-02-63	HWY: CTH F	COUNTY: EAU CLAIRE	CROSS SECTIONS: CTH F	SHEET E
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PROJECT NO: 7995-02-63      HWY: CTH F      COUNTY: EAU CLAIRE      CROSS SECTIONS: CTH F      SHEET      E



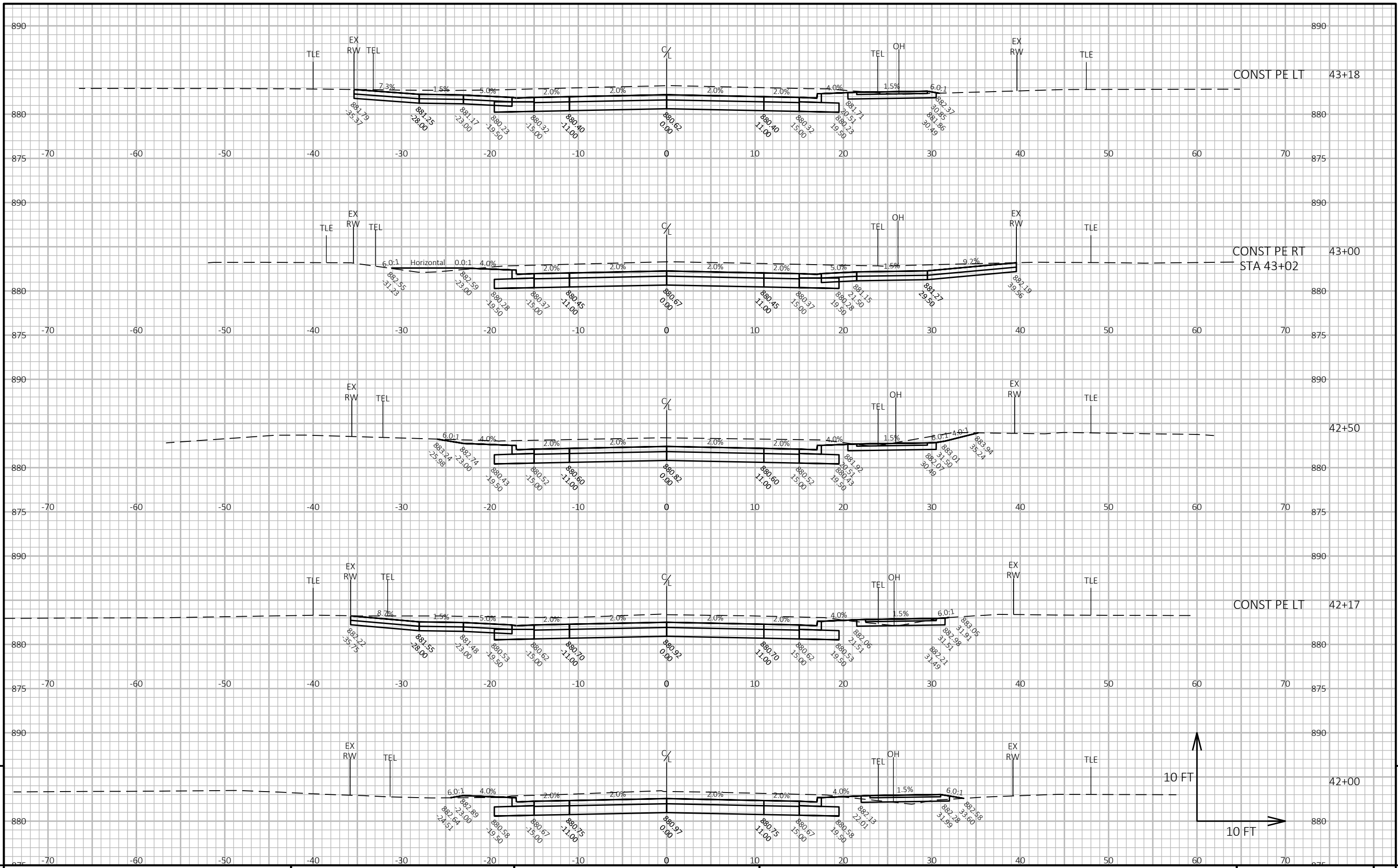
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FILE NAME: \\SEHINC.COM\PANZURA\PROJECTS\AE\EAUCH\165167\5-FINAL-DSGN\CIVIL 3D-STATE ST\DESIGN\CORRIDORS\CRDR-CTH F.DWG      PLOT DATE: 1/18/2024 7:35 PM      PLOT BY: JUSTIN SHAVLIK      PLOT NAME:      PLOT SCALE: 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.      WISDOT/CADD SHEET 49

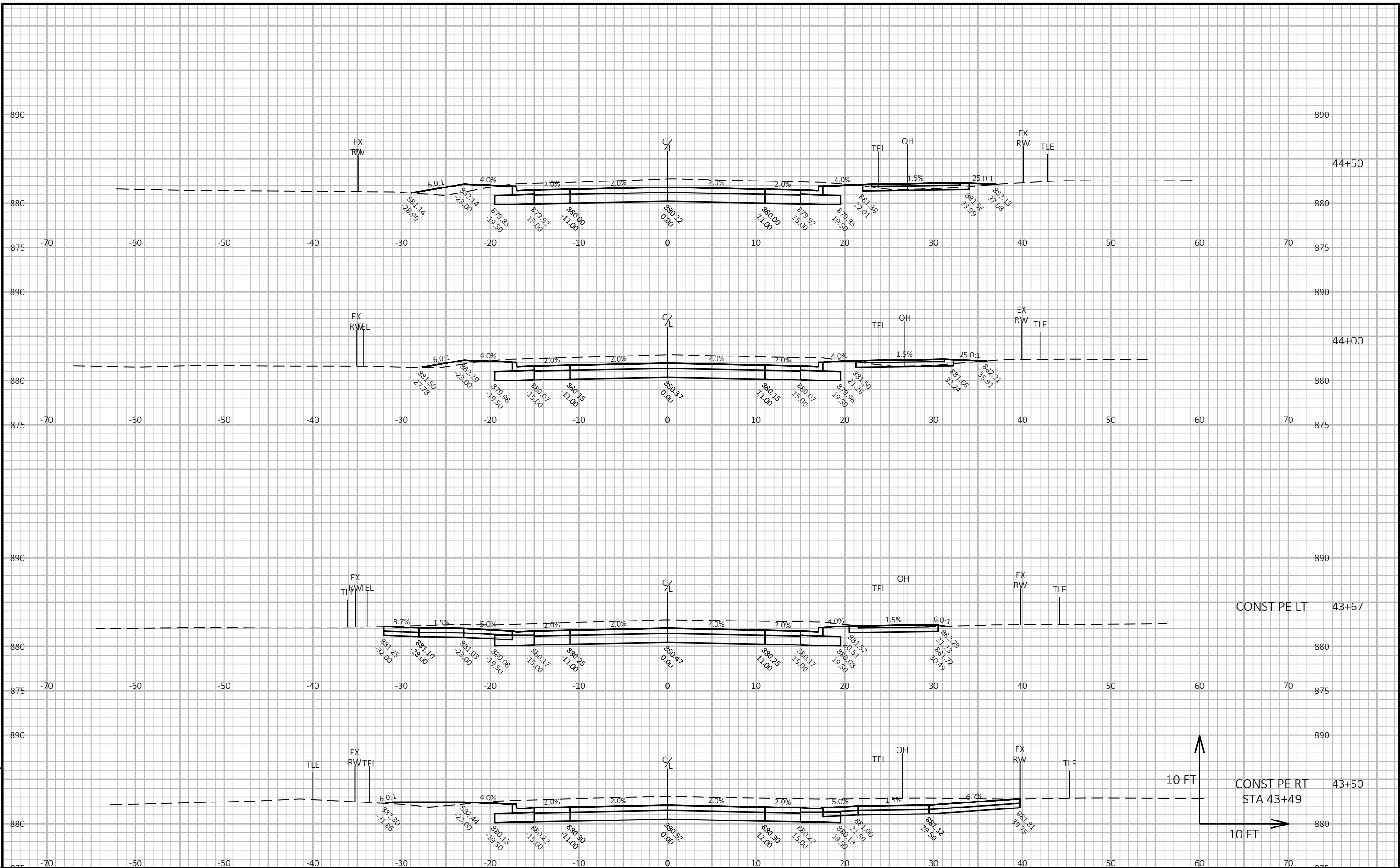
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9





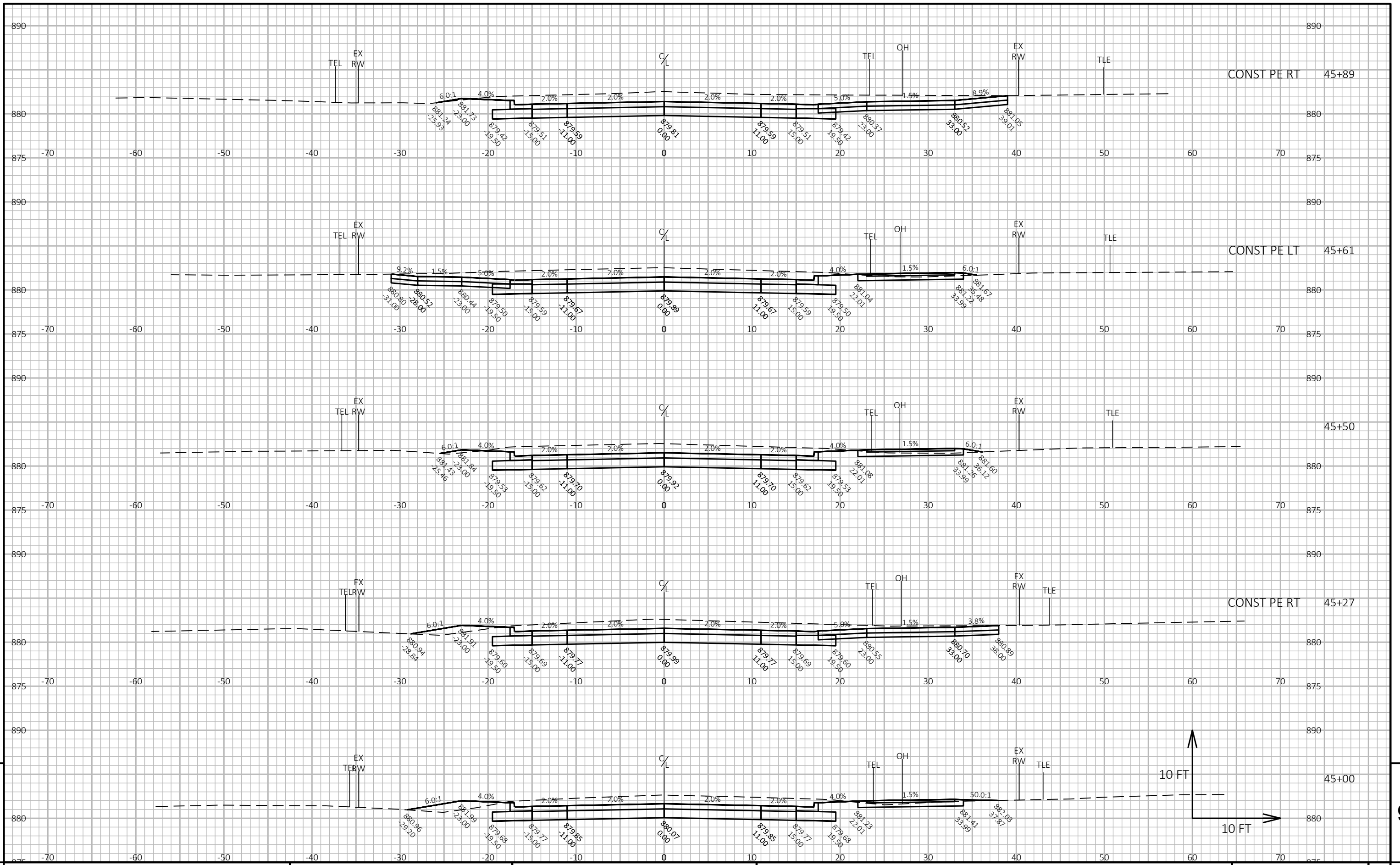
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PROJECT NO: 7995-02-63	HWY: CTH F	COUNTY: EAU CLAIRE	CROSS SECTIONS: CTH F	SHEET	E
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9

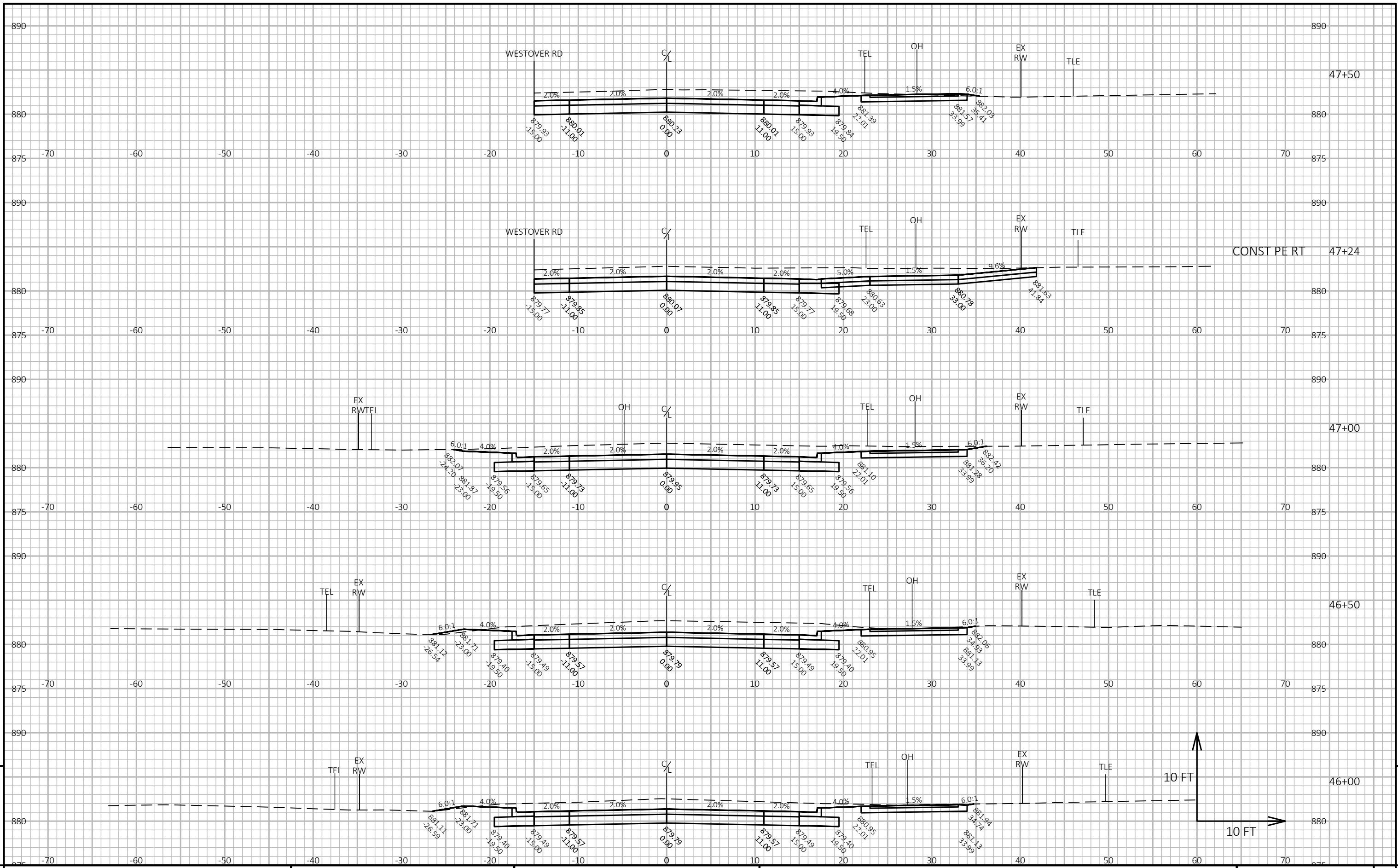
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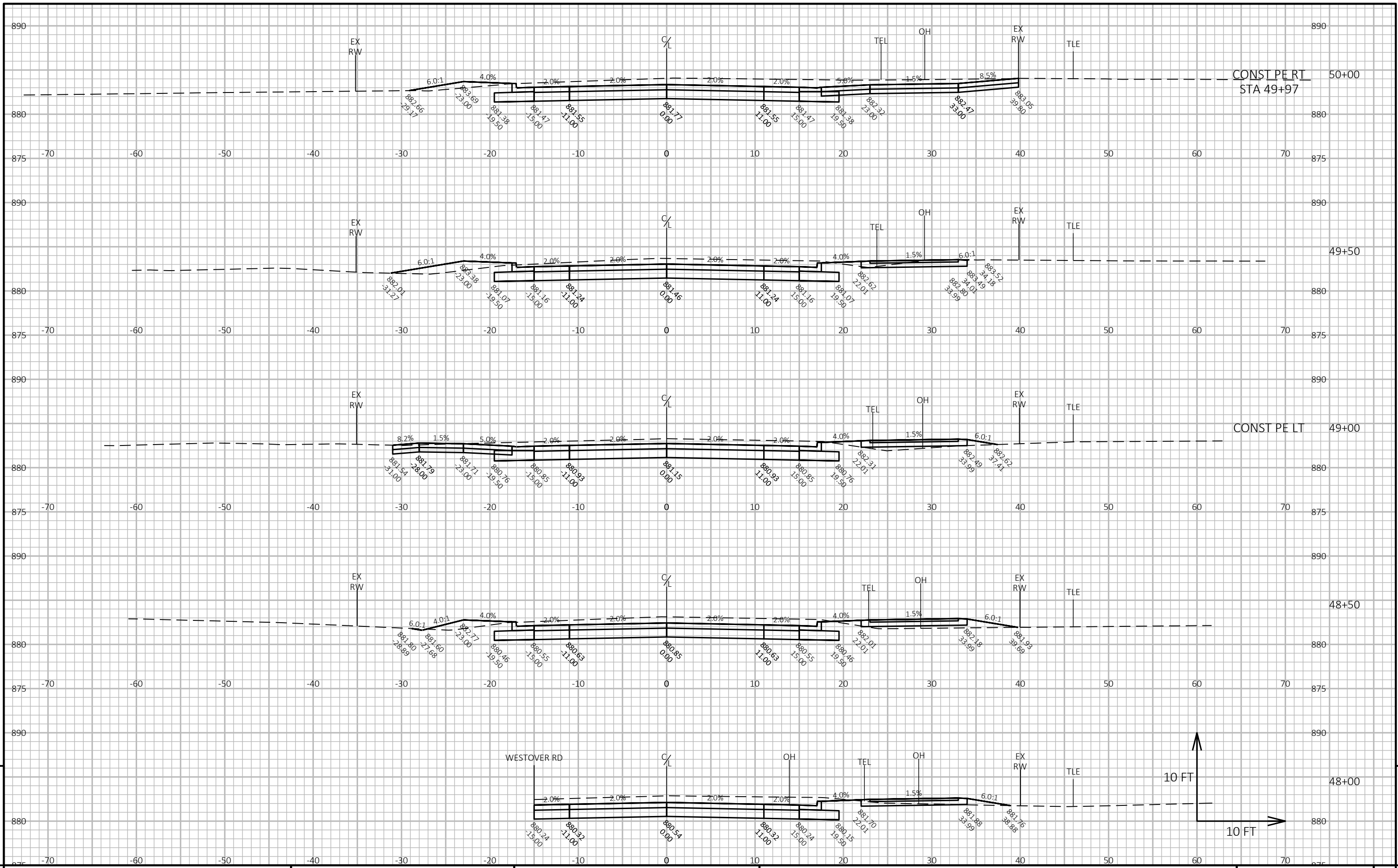
PROJECT NO: 7995-02-63      HWY: CTH F      COUNTY: EAU CLAIRE      CROSS SECTIONS: CTH F      SHEET      E

FILE NAME: \\SEHINC.COM\PANZURA\PROJECTS\AE\EAUCH\165167\5-FINAL-DSGN\CIVIL 3D-STATE ST\DESIGN\CORRIDORS\CRDR-CTH F.DWG      PLOT DATE: 1/18/2024 7:35 PM      PLOT BY: JUSTIN SHAVLIK      PLOT NAME:      PLOT SCALE: 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.      WISDOT/CADD SHEET 49

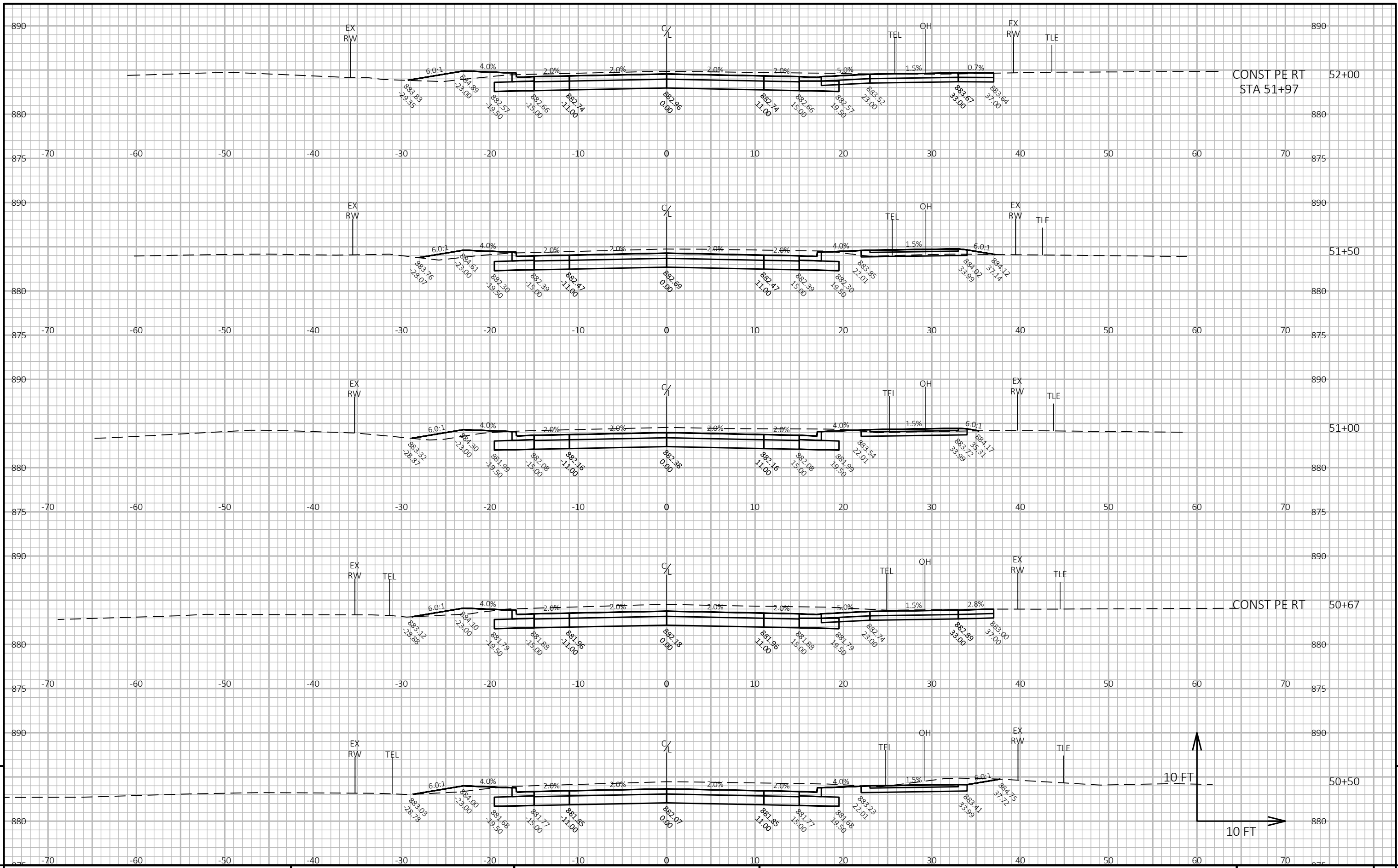
LAYOUT NAME - 12



PROJECT NO: 7995-02-63      HWY: CTH F      COUNTY: EAU CLAIRE      CROSS SECTIONS: CTH F      SHEET 9



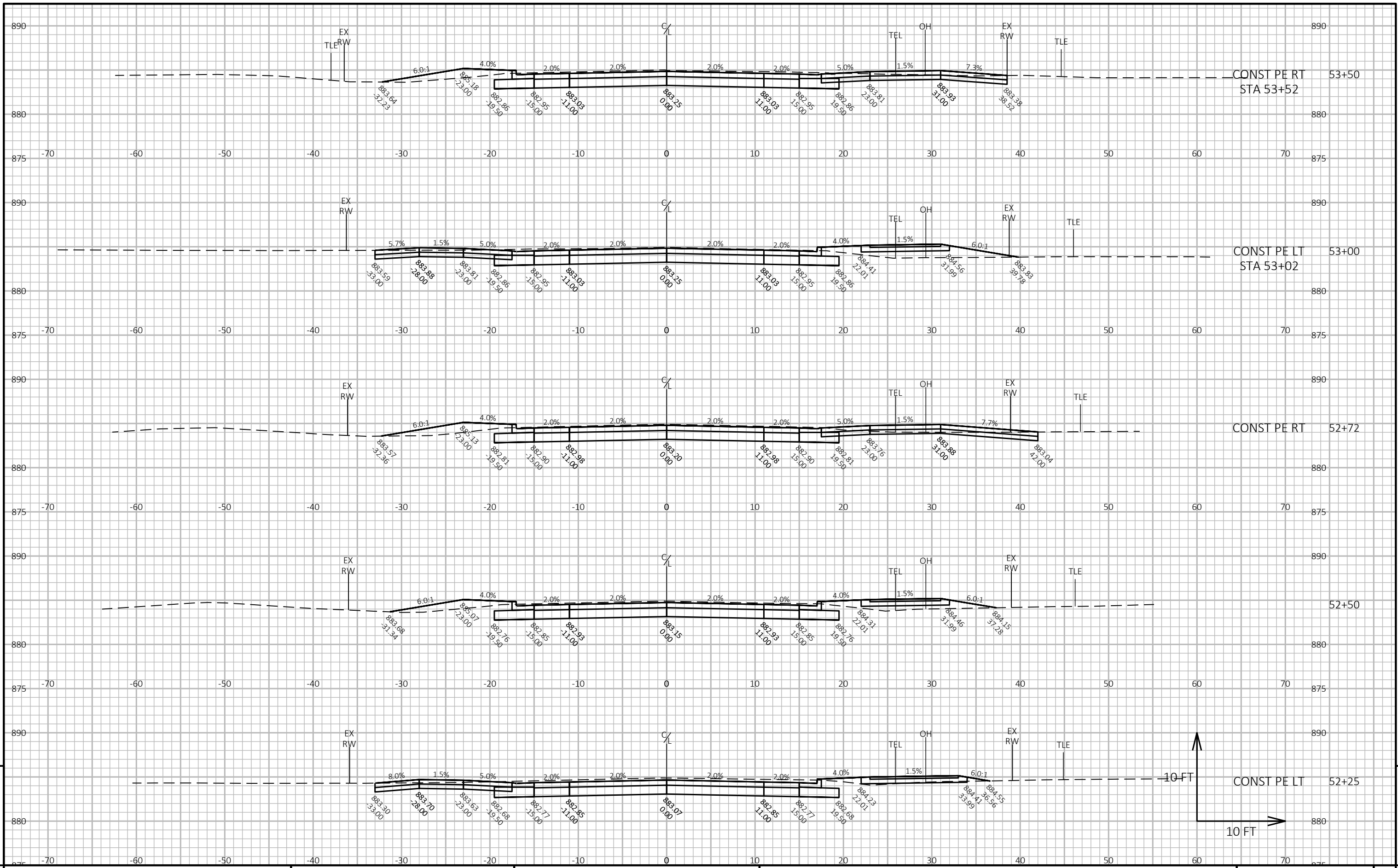
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PROJECT NO: 7995-02-63      HWY: CTH F      COUNTY: EAU CLAIRE      CROSS SECTIONS: CTH F      SHEET      E

FILE NAME: \\SEHINC.COM\PANZURA\P2\PROJECTS\AE\EAUCH\165167\5-FINAL-DSGN\CIVIL 3D-STATE ST\DESIGN\CORRIDORS\CRDR-CTH F.DWG      PLOT DATE: 1/18/2024 7:36 PM      PLOT BY: JUSTIN SHAVLIK      PLOT NAME:      PLOT SCALE: 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.      WISDOT/CADD SHEET 49

LAYOUT NAME - 15

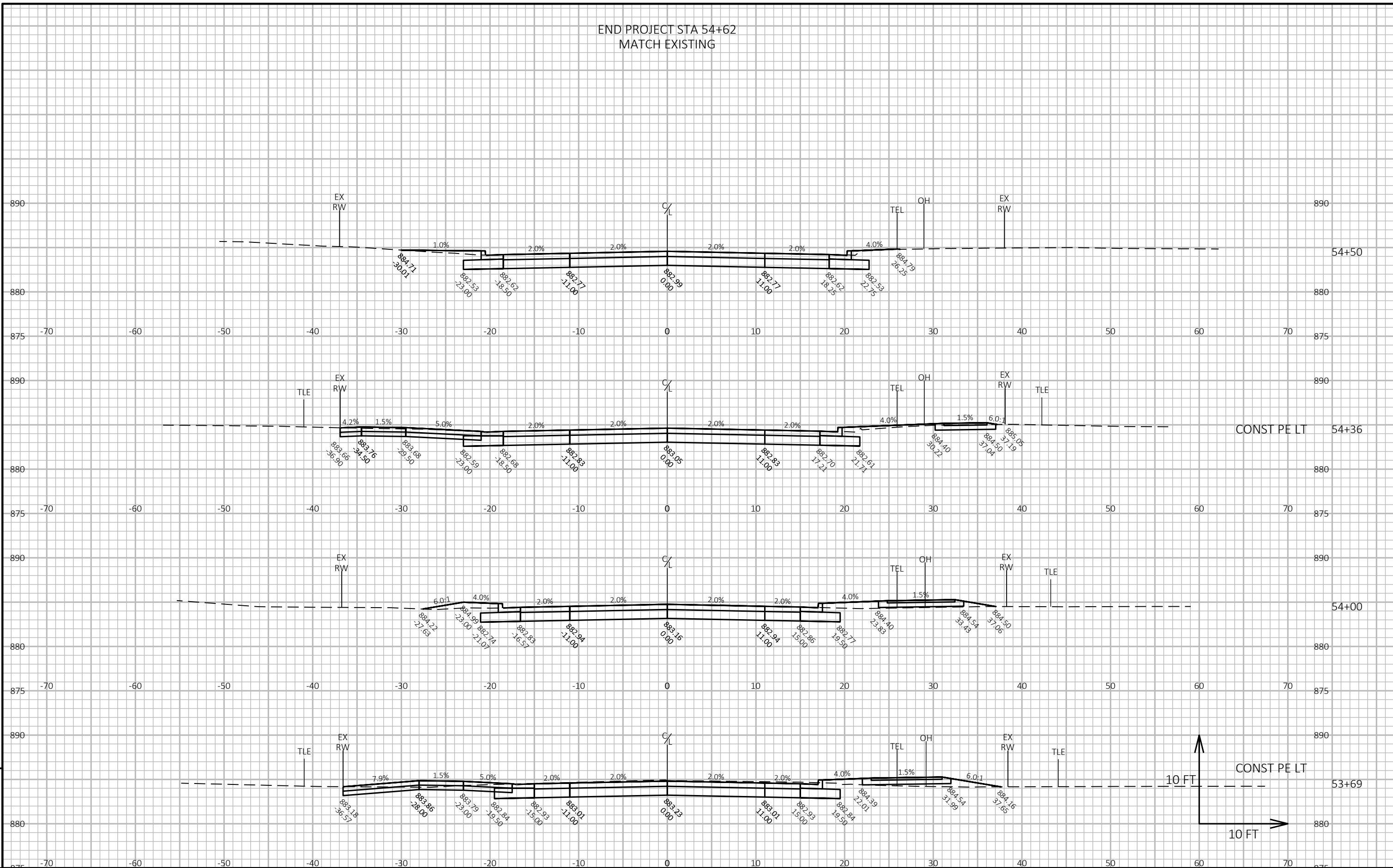


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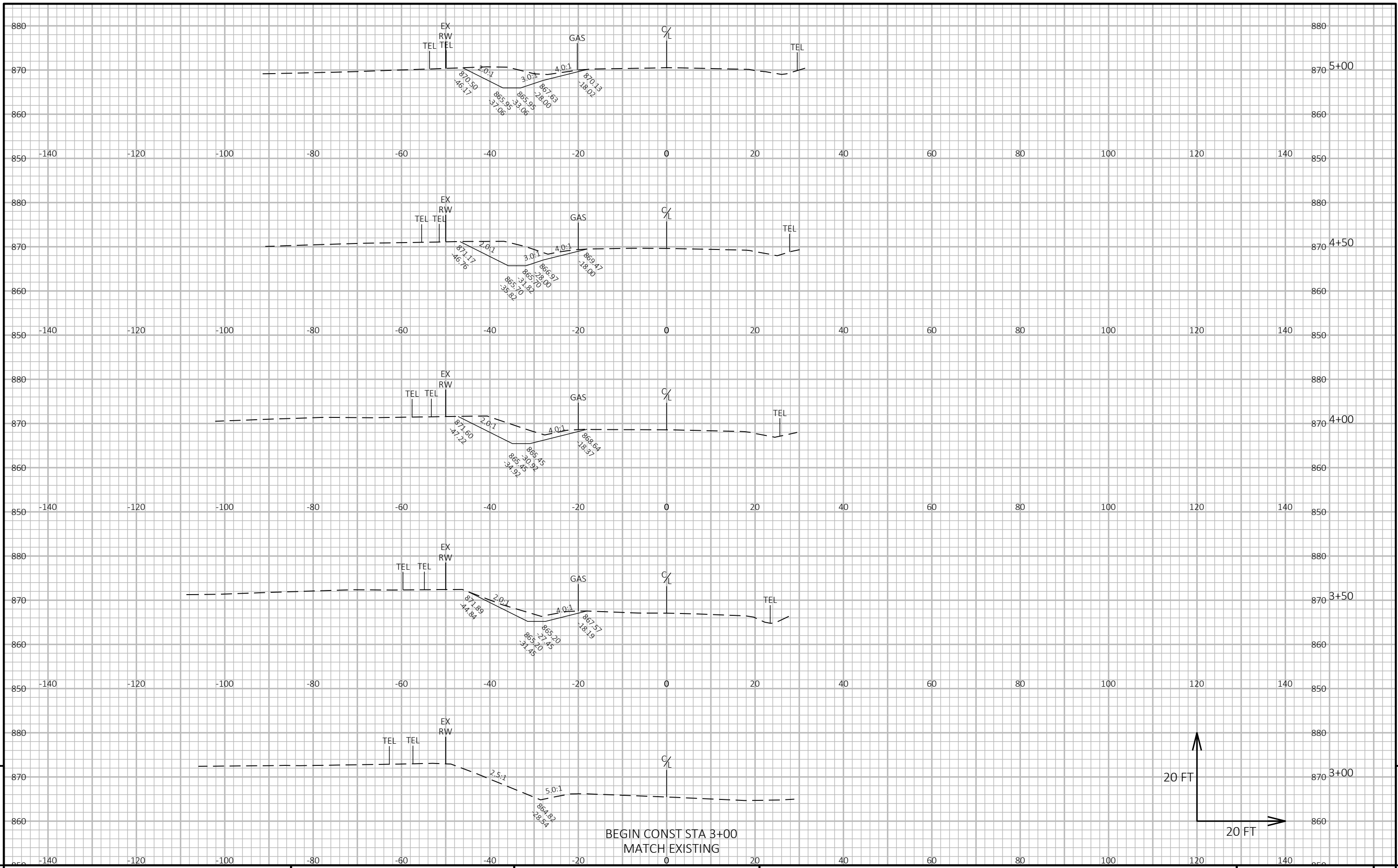
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LAYOUT NAME - 16

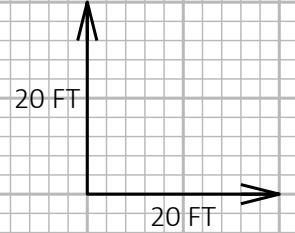
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MATCH EXISTING

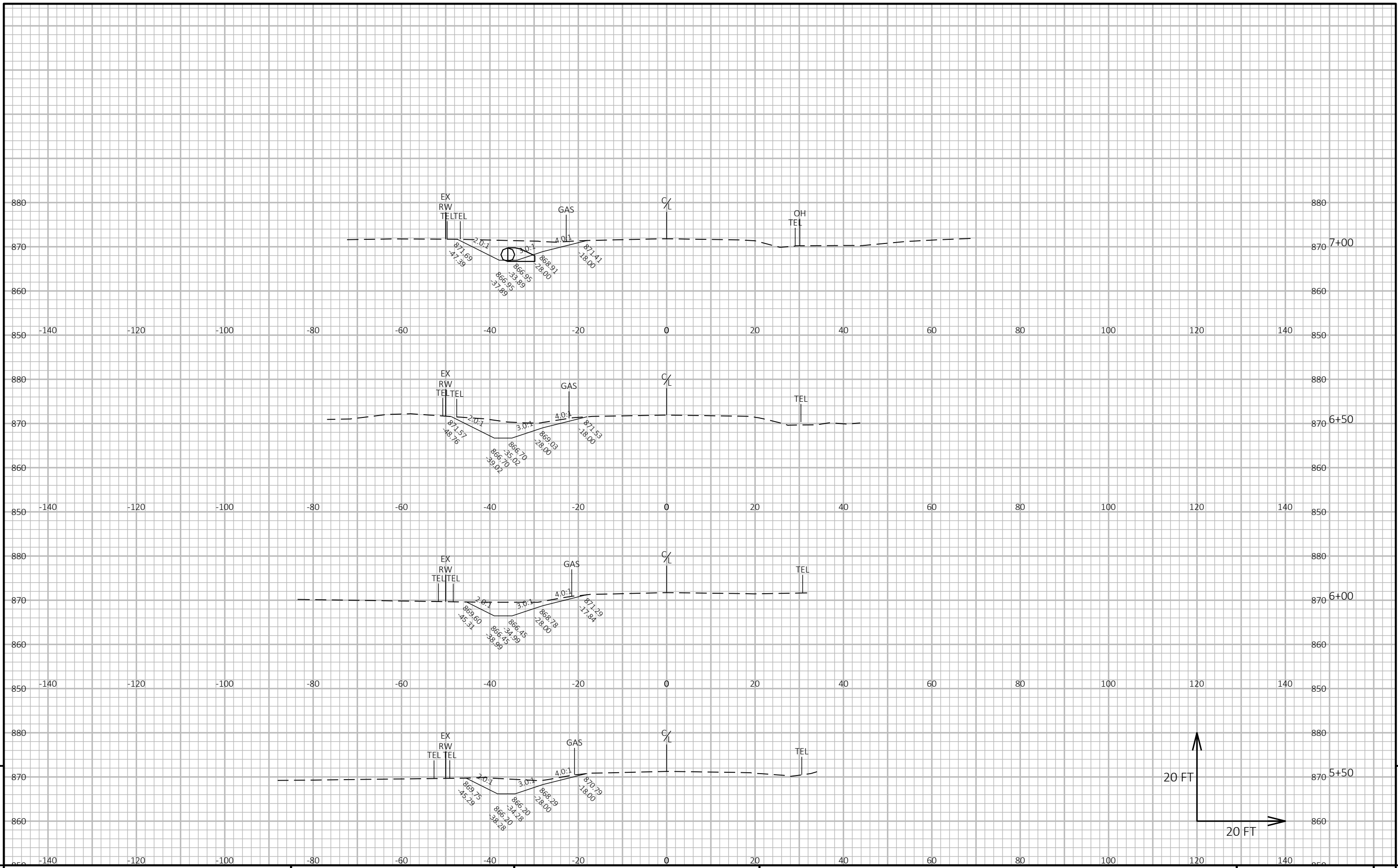






BEGIN CONST STA 3+00  
MATCH EXISTING





9

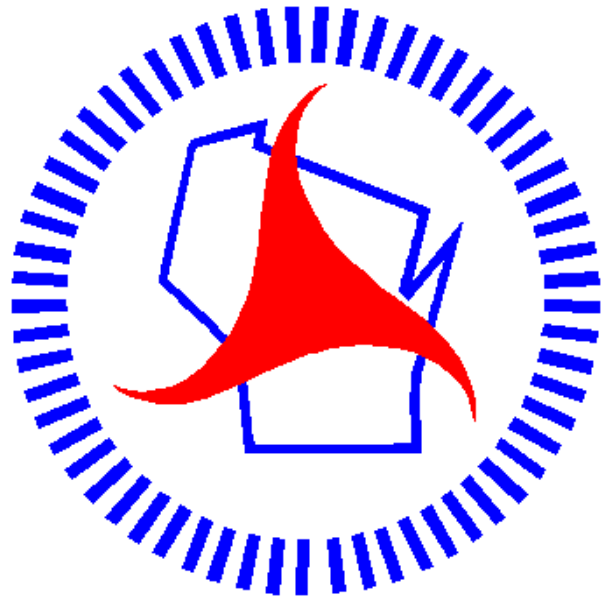
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PROJECT NO: 7995-02-63      HWY: CTH F      COUNTY: EAU CLAIRE      CROSS SECTIONS: W LOWES CREEK RD - SPECIAL DITCH      SHEET E

FILE NAME: \\SEHINC.COM\PANZURA\PROJECTS\AE\EAUCH\165167\5-FINAL-DSGN\CIVIL 3D-STATE ST\DESIGN\CORRIDORS\CRDR-DITCH.DWG      PLOT DATE: 1/18/2024 7:36 PM      PLOT BY: JUSTIN SHAVLIK      PLOT NAME:      PLOT SCALE: 1 IN:20 FT HORZ. / 1 IN:20 FT VERT.      WISDOT/CADD SHEET 49

LAYOUT NAME - SD 5

Notes



## ***Wisconsin Department of Transportation***

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