

ORDER OF SHEETS

Section No.	1	Title
Section No.	2	Typical Sections and Details
Section No.	3	Estimate of Quantities
Section No.	3	Miscellaneous Quantities
Section No.	4	Right of Way Plat
Section No.	5	Plan and Profile
Section No.	6	Standard Detail Drawings
Section No.	7	Sign Plates
Section No.	8	Structure Plans
Section No.	9	Computer Earthwork Data
Section No.	9	Cross Sections

TOTAL SHEETS = 102

PROJECT LOCATION



35

DESIGN DESIGNATION 8941-05-71

A.A.D.T.	2024	=	1325
A.A.D.T.	2044	=	1780
D.H.V.		=	178
D.D.		=	50/50
T.		=	10%
DESIGN SPEED		=	55 MPH
ESALS		=	440,000

BEGIN PROJECT
 STA 10+75
 Y = 396,137.291
 X = 585,792.082

CONVENTIONAL SYMBOLS

PLAN	
CORPORATE LIMITS	
PROPERTY LINE	
LOT LINE	
LIMITED HIGHWAY EASEMENT	
EXISTING RIGHT OF WAY	
PROPOSED OR NEW R/W LINE	
SLOPE INTERCEPT	
REFERENCE LINE	
EXISTING CULVERT	
PROPOSED CULVERT (Box or Pipe)	
COMBUSTIBLE FLUIDS	
MARSH AREA	
WOODED OR SHRUB AREA	

PROFILE	
GRADE LINE	
ORIGINAL GROUND	
MARSH OR ROCK PROFILE (To be noted as such)	
SPECIAL DITCH	
GRADE ELEVATION	
CULVERT (Profile View)	
UTILITIES	
ELECTRIC	
FIBER OPTIC	
GAS	
SANITARY SEWER	
STORM SEWER	
TELEPHONE	
WATER	
UTILITY PEDESTAL	
POWER POLE	
TELEPHONE POLE	

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

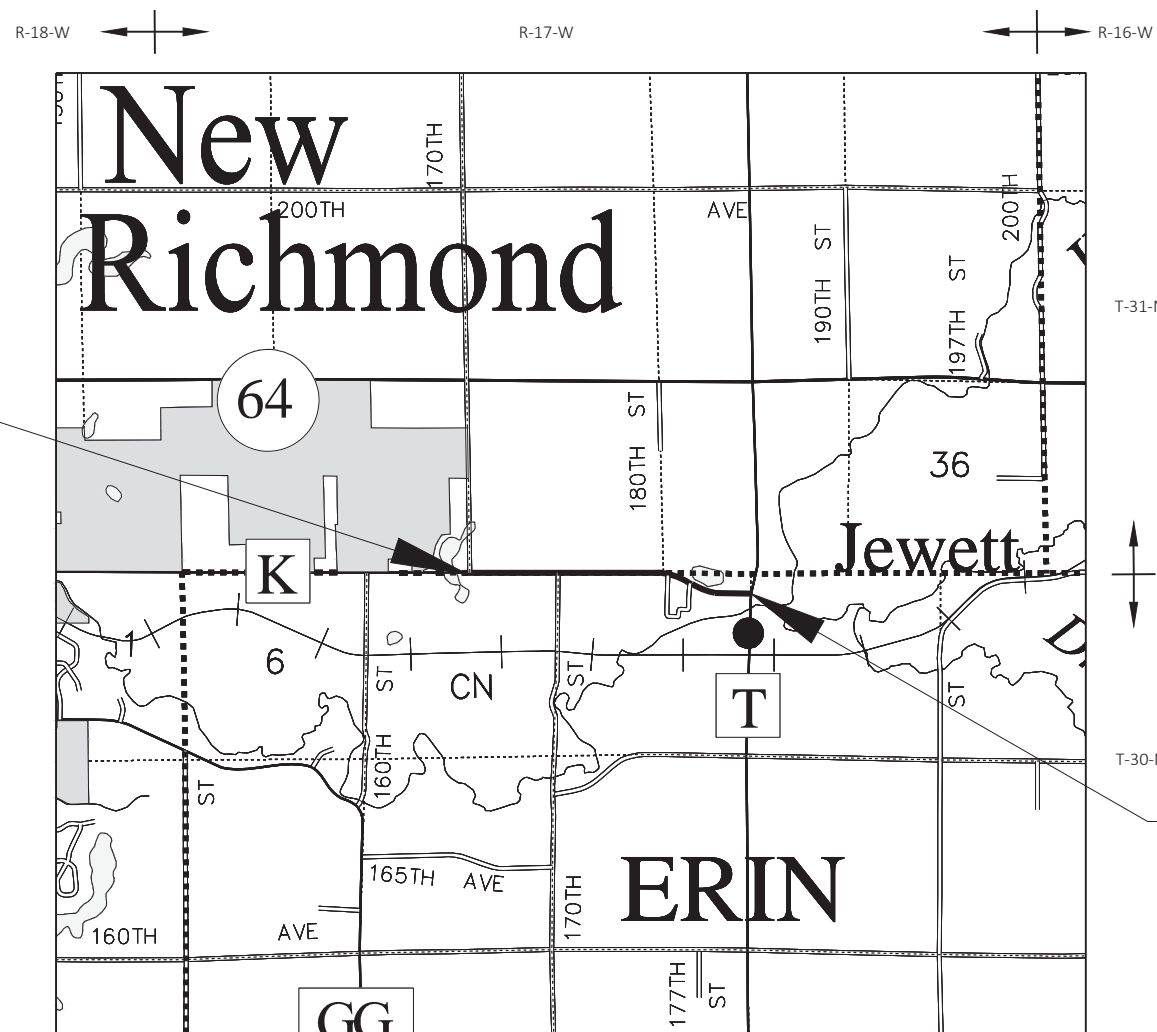
NEW RICHMOND - JEWETT

170TH STREET NORTH TO CTH T

CTH K

ST CROIX COUNTY

STATE PROJECT NUMBER
8941-05-71



LAYOUT
 SCALE 0 1 MI
 TOTAL NET LENGTH OF CENTERLINE = 1.492 MILES

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS), ST CROIX COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES. ELEVATIONS ARE REFERENCED TO NAVD 88 (2012). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 12A.

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
8941-05-71	WISC 2024284	1

ACCEPTED FOR
 ST. CROIX COUNTY
 HIGHWAY DEPARTMENT
 Gregory Haig
Digitally signed by Gregory Haig
 DN: CN = Gregory Haig email = greg.haig@scowi.gov C = US O = St. Croix County Highway OU = Engineering
 Date: 2023.10.04.14:49:05 -0500
 (Highway Department Engineer Signature)

ORIGINAL PLANS PREPARED BY

STATE OF WISCONSIN
 DEPARTMENT OF TRANSPORTATION

PREPARED BY	
Surveyor	SEH, WISDOT
Designer	SEH
Project Manager	MATTHEW BERG
Regional Examiner	TOU YANG
Regional Supervisor	TYLER RONGSTAD

APPROVED FOR THE DEPARTMENT
 10/5/2023
 Matthew Berg
Digitally signed by Matthew Berg
 DN: CN = Matthew Berg email = matthew.berg@dot.wis.gov
 O = Wisconsin Department of Transportation
 OU = Regional Project Manager, CH-Matthew Berg
 Date: 2023.10.05.07:05:24 -0500
 DATE: _____ (Signature)

E

GENERAL NOTES

WHEN THE QUANTITY OF HMA PAVEMENT OR BASE AGGREGATE DENSE IS MEASURED BY THE TON, THE DEPTH OR THICKNESS SHOWN ON THE PLAN IS APPROXIMATE AND THE ACTUAL THICKNESS WILL DEPEND ON THE DISTRIBUTION OF THE MATERIAL AS DIRECTED BY THE ENGINEER.

THE LOCATION OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS IS APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

NO TREES OR SHRUBS ARE TO BE REMOVED WITHOUT THE APPROVAL OF THE ENGINEER.

THE EXACT LOCATION OF THE EROSION CONTROL DEVICES SHALL BE DETERMINED IN THE FIELD.

DISTURBED AREAS WITHIN THE RIGHT-OF-WAY, EXCEPT THE AREAS WITHIN THE FINISHED SHOULDER POINTS, ARE TO BE FERTILIZED, SEEDED, AND MULCHED.

ALL CURB AND GUTTER RADII, PAVEMENT DIMENSIONS AND STATIONS ARE SHOWN TO THE EDGE OF PAVEMENT UNLESS NOTED OTHERWISE.

ALL SIDE ROAD EARTHWORK QUANTITIES ARE INCLUDED IN MAINLINE EARTHWORK QUANTITIES.

A VERTICAL SAWCUT SHALL BE MADE THROUGH EXISTING DRIVEWAYS AND PAVEMENTS AT REMOVAL LIMITS.

ACCESS TO ALL RESIDENCES SHALL BE MAINTAINED DURING CONSTRUCTION.

RESTORE SIDE ROAD INTERSECTIONS AND PRIVATE ENTRANCES TO EXISTING CONDITIONS UNLESS OTHERWISE SHOWN.

THE EXACT CONSTRUCTION LIMITS OF PRIVATE ENTRANCES SHALL BE COORDINATED WITH THE ENGINEER IN THE FIELD.

EXISTING PIPE CULVERT SIZES SHOWN ARE APPROXIMATE AND THE CONTRACTOR SHALL BASE ITS BID ON ACTUAL FIELD CONDITIONS.

UTILITY CONTACTS

FRONTIER COMMUNICATIONS - COMMUNICATIONS
1851 N 14TH AVENUE
WAUSAU, WI 54401
TELEPHONE: 608.844.0980
ATTENTION: THOMAS REKOWSKI
EMAIL: THOMAS.REKOWSKI@FTR.COM

ST CROIX ELECTRIC COOPERATIVE - ELECTRICITY
1925 RIDGEWAY STREET
HAMMOND, WI 54015
TELEPHONE: 715.796.5633
ATTENTION: BRIAN FERG
EMAIL: BRIANF@SCECNET.NET

XCEL ENERGY - GAS/PETROLEUM
2001 OLD STATE HWY 35 S
HUDSON WI, 54016
TELEPHONE: 715.232.7446 (OFFICE), 715.403.8446 (MOBILE)
ATTENTION: CHRISTINE CARLI
EMAIL: CHRISTINE.J.CARLI@XCELENERGY.COM



Dial  or (800)242-8511
www.DiggersHotline.com

RUNOFF COEFFICIENT TABLE

	HYDROLOGIC SOIL GROUP											
	A			B			C			D		
	SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)		
LAND USE:	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER
ROW CROPS	.08 .22	.16 .30	.22 .38	.12 .26	.20 .34	.27 .44	.15 .30	.24 .37	.33 .50	.19 .34	.28 .41	.38 .56
MEDIAN STRIP-TURF	.19 .24	.20 .26	.24 .30	.19 .25	.22 .28	.26 .33	.20 .26	.23 .30	.30 .37	.20 .27	.25 .32	.30 .40
SIDE SLOPE-TURF			.25 .32			.27 .34			.28 .36			.30 .38
PAVEMENT:												
ASPHALT	.70 - .95											
CONCRETE	.80 - .95											
BRICK	.70 - .80											
DRIVES, WALKS	.75 - .85											
ROOFS	.75 - .95											
GRAVEL ROADS, SHOULDERS	.40 - .60											

DESIGN CONTACT
SEH
10 NORTH BRIDGE STREET
CHIPPEWA FALLS, WI 54279
TELEPHONE: 715.720.6279
ATTENTION: JUSTIN SHAVLIK
EMAIL: JSHAVLIK@SEHINC.COM

OWNER CONTACT
ST. CROIX COUNTY HIGHWAY DEPARTMENT
300 OAK RIDGE PKWY
BALDWIN, WI 54002
TELEPHONE: 715.245.4205
ATTENTION: GREG HAIG
EMAIL: GREG.HAIG@SCCWI.GOV

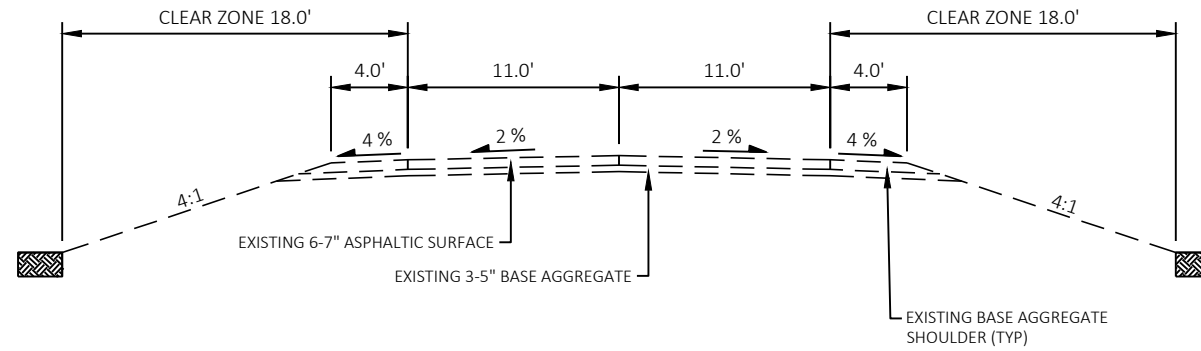
WISDOT CONTACT
WISDOT NW REGION
718 W CLAIREMONT AVE.
EAU CLAIRE, WI 54701
TELEPHONE: 920.366.4750
ATTENTION: MATTHEW BERG
EMAIL: MATTHEW.BERG@DOT.WI.COM

WDNR CONTACT
WDNR WEST CENTRAL REGION HQ
1300 WEST CLAIREMONT AVE
EAU CLAIRE, WI 54701
TELEPHONE 715.635.4229
ATTENTION: AMY LESIK
EMAIL: AMYL.LESIK@WISCONSIN.GOV

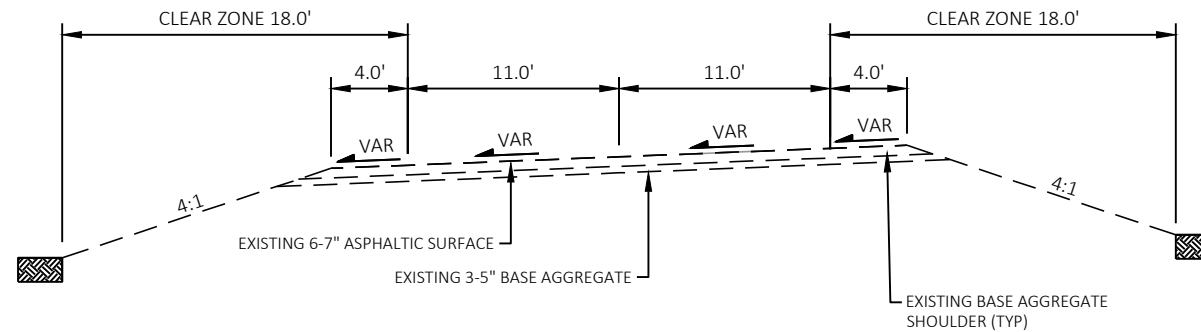
TOTAL PROJECT AREA = 18.0 ACRES
TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = 4.2 ACRES



PROJECT NO: 8941-05-71	HWY: CTH K	COUNTY: ST CROIX	PROJECT OVERVIEW	SHEET	E
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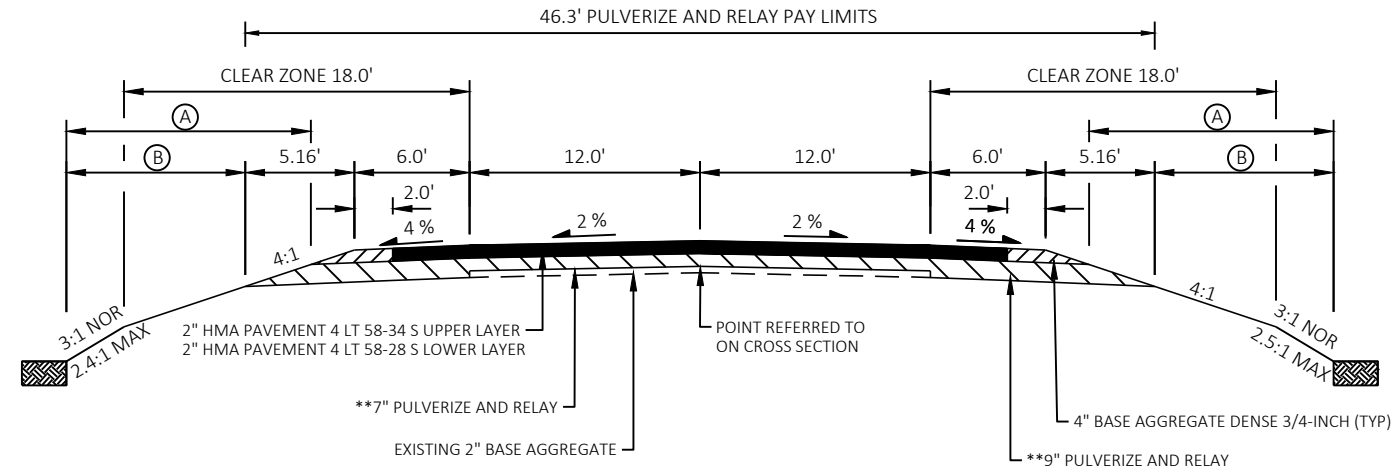


EXISTING TYPICAL SECTION
 CTH K
 STA 10+75 - STA 89+53.52



EXISTING TYPICAL SECTION
 CTH K
 SEE SUPERELEVATION TABLE

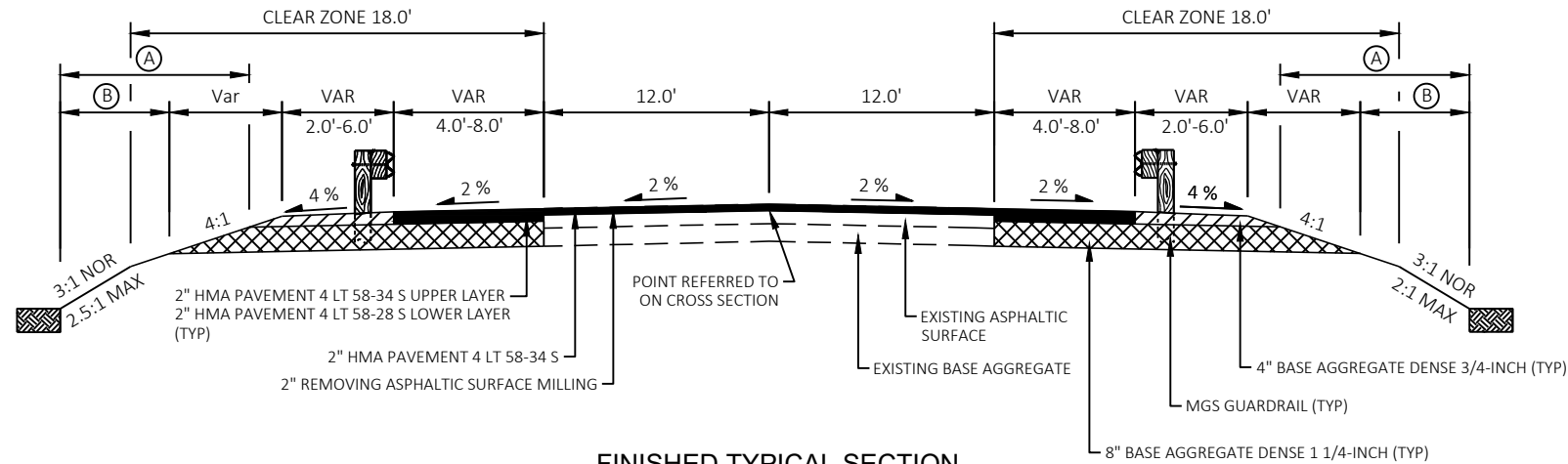
PAVEMENT CORE LOCATION			
CORE	STATION	ASPHALT THICKNESS IN	BASE THICKNESS IN
B-1	28+75	7"	5"
B-2	52+25	6"	3"
B-3	73+50	6"	3"



FINISHED TYPICAL SECTION

CTH K
STA 10+75 - STA 17+80
STA 22+50 - STA 89+53.52

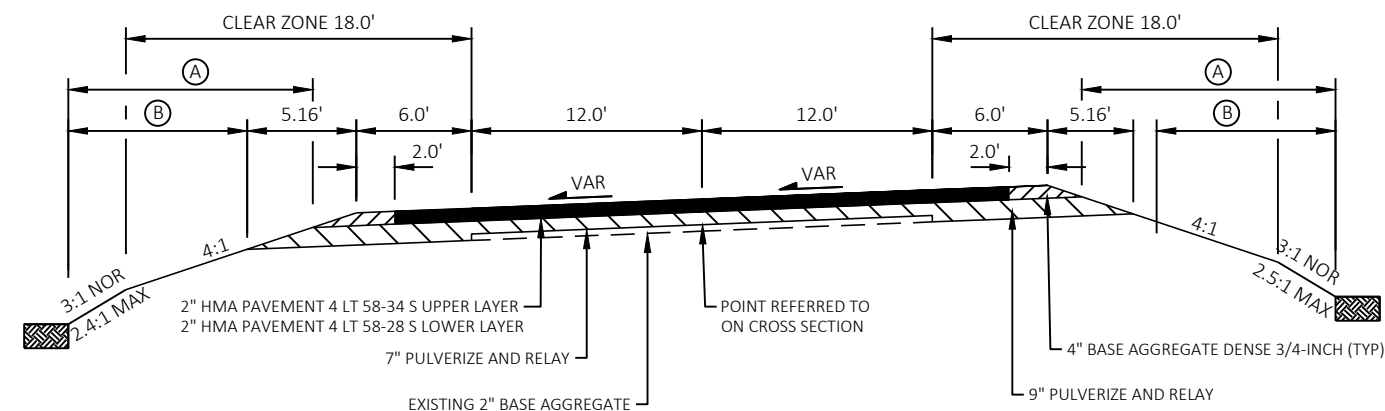
** PRIOR TO PULVERIZING PLACE 2" OF NEW BASE AGGREGATE DENSE 1 1/4-INCH (PAID AS BASE AGGREGATE DENSE 1 1/4-INCH) ATOP EXISTING ASPHALT SURFACE AND EXCAVATE AND SPREAD THE EXISTING BASE SHOULDER MATERIAL ATOP EXISTING ASPHALT SURFACE. PULVERIZE AT LEAST 1.0" INTO EXISTING BASE AGGREGATE MATERIAL. DEPTH OF PULVERIZING VARIES BY LOCATION. SEE PAVEMENT CORE TABLE FOR EXISTING LAYER THICKNESS.



FINISHED TYPICAL SECTION

CTH K
STA 17+80 - STA 22+50

- (A) SEEDING MIXTURE NO. 20, TEMPORARY SEED, AND FERTILIZER TYPE B
- (B) SALVAGED TOPSOIL AND MULCHING (MULCHING NOT REQ'D WHERE EMAT IS SHOWN ON EROSION CONTROL SHEETS)



FINISHED TYPICAL SUPERELEVATED SECTION

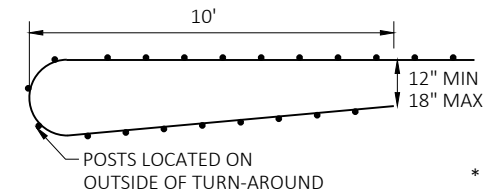
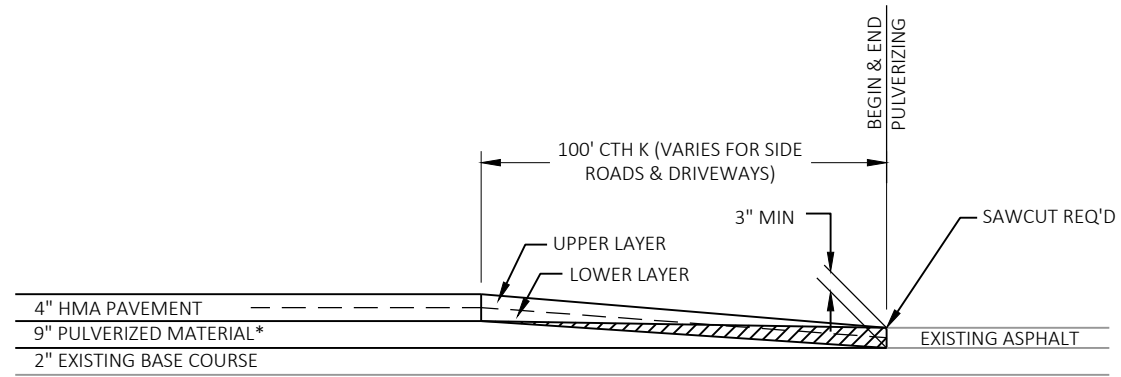
CTH K
SEE SUPERELEVATION TABLE

ALIGNMENT DATA

TANGENT DATA			
DESCRIPTION	PT STATION	NORTHING	EASTING
START:	2+43.097	396143.585	585140.203
END:	10+59.739	396137.286	585956.82
TANGENT DATA			
PARAMETER	VALUE	PARAMETER	VALUE
LENGTH:	816.641	COURSE:	S 89° 33' 28.8746" E
TANGENT DATA			
DESCRIPTION	PT STATION	NORTHING	EASTING
START:	10+59.739	396137.286	585956.82
END:	20+05.760	396137.625	586902.841
TANGENT DATA			
PARAMETER	VALUE	PARAMETER	VALUE
LENGTH:	946.021	COURSE:	N 89° 58' 45.9532" E
TANGENT DATA			
DESCRIPTION	PT STATION	NORTHING	EASTING
START:	20+05.760	396137.625	586902.841
END:	27+34.400	396140.473	587631.475
TANGENT DATA			
PARAMETER	VALUE	PARAMETER	VALUE
LENGTH:	728.64	COURSE:	N 89° 46' 33.7434" E
TANGENT DATA			
DESCRIPTION	PT STATION	NORTHING	EASTING
START:	27+34.400	396140.473	587631.475
END:	62+76.117	396141.485	591173.193
TANGENT DATA			
PARAMETER	VALUE	PARAMETER	VALUE
LENGTH:	3541.718	COURSE:	N 89° 59' 01.1005" E
CURVE POINT DATA			
DESCRIPTION	STATION	NORTHING	EASTING
PC:	62+76.117	396141.485	591173.193
PI:	66+66.05	394504.462	591173.661
PT:	70+41.707	395965.912	591911.229
CIRCULAR CURVE DATA			
PARAMETER	VALUE	PARAMETER	VALUE
DELTA:	26° 47' 44.3276"	TYPE:	RIGHT
RADIUS:	1637.022		
LENGTH:	765.59	TANGENT:	389.928
MID-ORD:	44.552	EXTERNAL:	45.799
CHORD:	758.632	COURSE:	S 76° 37' 06.7357" E
TANGENT DATA			
DESCRIPTION	PT STATION	NORTHING	EASTING
START:	70+41.707	395965.912	591911.229
END:	75+32.163	395744.935	592349.083
TANGENT DATA			
PARAMETER	VALUE	PARAMETER	VALUE
LENGTH:	490.456	COURSE:	S 63° 13' 14.5719" E
CURVE POINT DATA			
DESCRIPTION	STATION	NORTHING	EASTING
PC:	75+32.163	395744.935	592349.083
PI:	79+20.41	397206.384	593086.651
PT:	82+94.575	395569.364	593083.94
CIRCULAR CURVE DATA			
PARAMETER	VALUE	PARAMETER	VALUE
DELTA:	26° 41' 03.8511"	TYPE:	LEFT
RADIUS:	1637.022		
LENGTH:	762.411	TANGENT:	388.249
MID-ORD:	44.185	EXTERNAL:	45.41
CHORD:	755.54	COURSE:	S 76° 33' 46.4975" E
TANGENT DATA			
DESCRIPTION	PT STATION	NORTHING	EASTING
START:	82+94.575	395569.364	593083.94
END:	90+36.293	395568.136	593825.657
TANGENT DATA			
PARAMETER	VALUE	PARAMETER	VALUE
LENGTH:	741.718	COURSE:	S 89° 54' 18.4230" E

SUPERELEVATION DATA

STATION	DESCRIPTION	LEFT OUTSIDE SHOULDER	LEFT OUTSIDE LANE	RIGHT OUTSIDE LANE	RIGHT OUTSIDE SHOULDER
61+33.00'	END NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
61+33.00'	END NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
61+63.67'	MANUAL STATION	-1.60%	-0.80%	-2.00%	-4.00%
61+84.11'	LEVEL CROWN	-1.60%	0.00%	-2.00%	-4.00%
62+35.22'	REVERSE CROWN	-1.60%	2.00%	-2.00%	-4.00%
62+86.33'	LOW SHOULDER MATCH	-1.60%	4.00%	-4.00%	-4.00%
63+22.11'	BEGIN FULL SUPER	-1.60%	5.40%	-5.40%	-5.40%
69+95.71'	END FULL SUPER	-1.60%	5.40%	-5.40%	-5.40%
70+31.49'	LOW SHOULDER MATCH	-1.60%	4.00%	-4.00%	-4.00%
70+82.60'	REVERSE CROWN	-1.60%	2.00%	-2.00%	-4.00%
71+33.71'	LEVEL CROWN	-1.60%	0.00%	-2.00%	-4.00%
71+54.16'	MANUAL STATION	-1.60%	-0.80%	-2.00%	-4.00%
71+84.82'	BEGIN NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
71+84.82'	BEGIN NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
73+89.05'	END NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%
73+89.05'	END NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
74+40.16'	LEVEL CROWN	-4.00%	-2.00%	0.00%	0.00%
74+91.27'	REVERSE CROWN	-4.00%	-2.00%	2.00%	2.00%
75+42.38'	LOW SHOULDER MATCH	-4.00%	-4.00%	4.00%	4.00%
75+78.16'	BEGIN FULL SUPER	-5.40%	-5.40%	5.40%	5.40%
82+48.58'	END FULL SUPER	-5.40%	-5.40%	5.40%	5.40%
82+84.36'	LOW SHOULDER MATCH	-4.00%	-4.00%	4.00%	4.00%
83+35.47'	REVERSE CROWN	-4.00%	-2.00%	2.00%	2.00%
83+86.58'	LEVEL CROWN	-4.00%	-2.00%	0.00%	0.00%
84+37.69'	BEGIN NORMAL CROWN	-4.00%	-2.00%	-2.00%	-4.00%
84+37.69'	BEGIN NORMAL SHOULDER	-4.00%	-2.00%	-2.00%	-4.00%



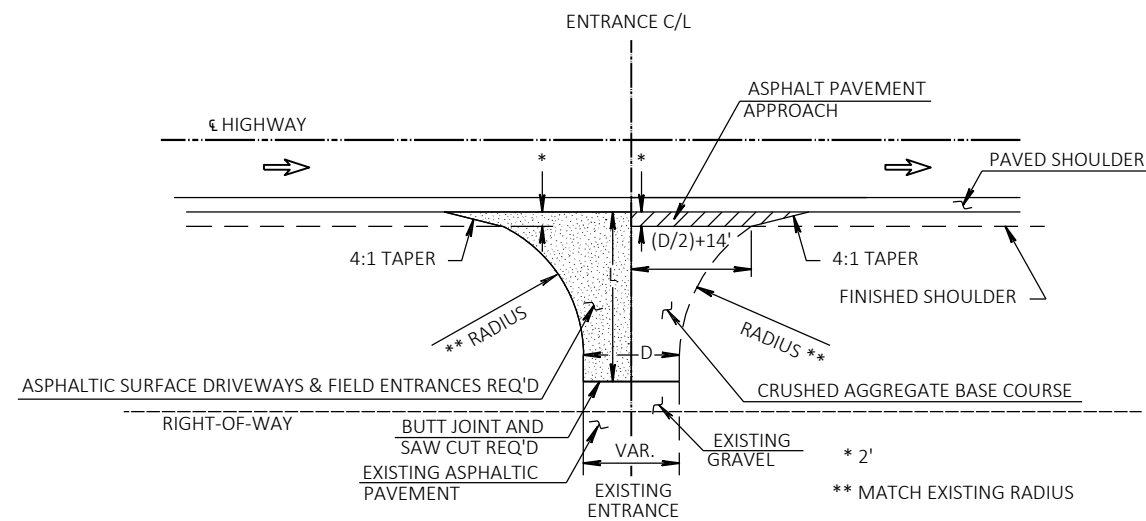
* INSTALL AS SHOWN ON PLANS PRIOR TO ANY DISTURBANCE. EXCLUSION FENCING TO REMAIN IN PLACE UNTIL ALL DISTURBANCE IS COMPLETE.

AMPHIBIAN AND REPTILE EXCLUSION
FENCING TURN-AROUND DETAIL*

REMOVING ASPHALTIC SURFACE BUTT JOINTS

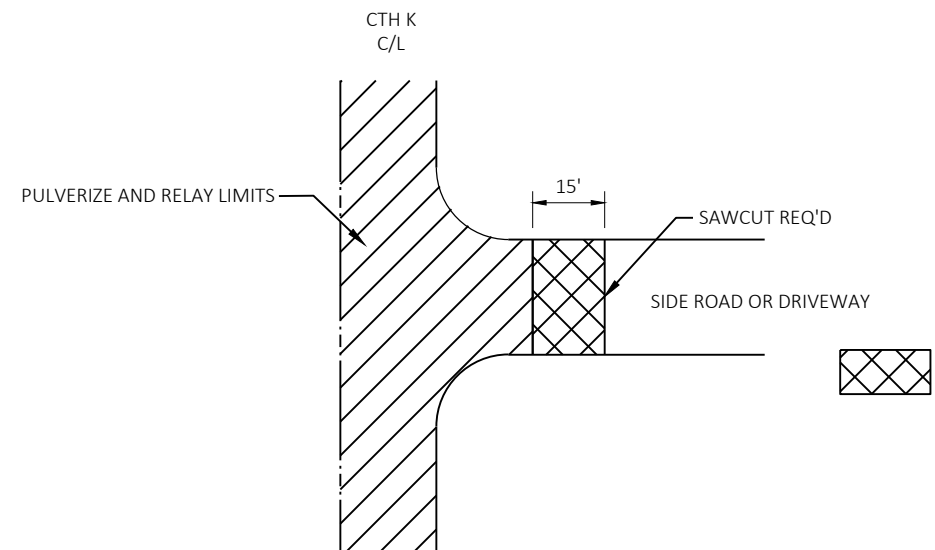
* PLACE 2" BASE AGGREGATE DENSE 1 1/4-INCH AND EXISTING SHOULDER GRAVEL ATOP EXISTING HMA PRIOR TO PULVERIZING. SEE TYPICAL SECTIONS.

BUTT JOINT DETAIL



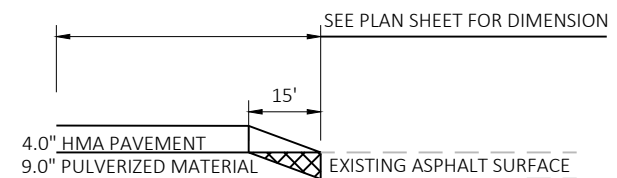
L=VARIABLE, EXACT LENGTH TO BE DETERMINED IN THE FIELD BY THE ENGINEER. BLEND BACK ON THE ENTRANCE FAR ENOUGH TO GET A SMOOTH PROFILE.
D=DRIVEWAY WIDTH, MATCH EXISTING

PROPOSED RURAL DRIVEWAY DETAIL

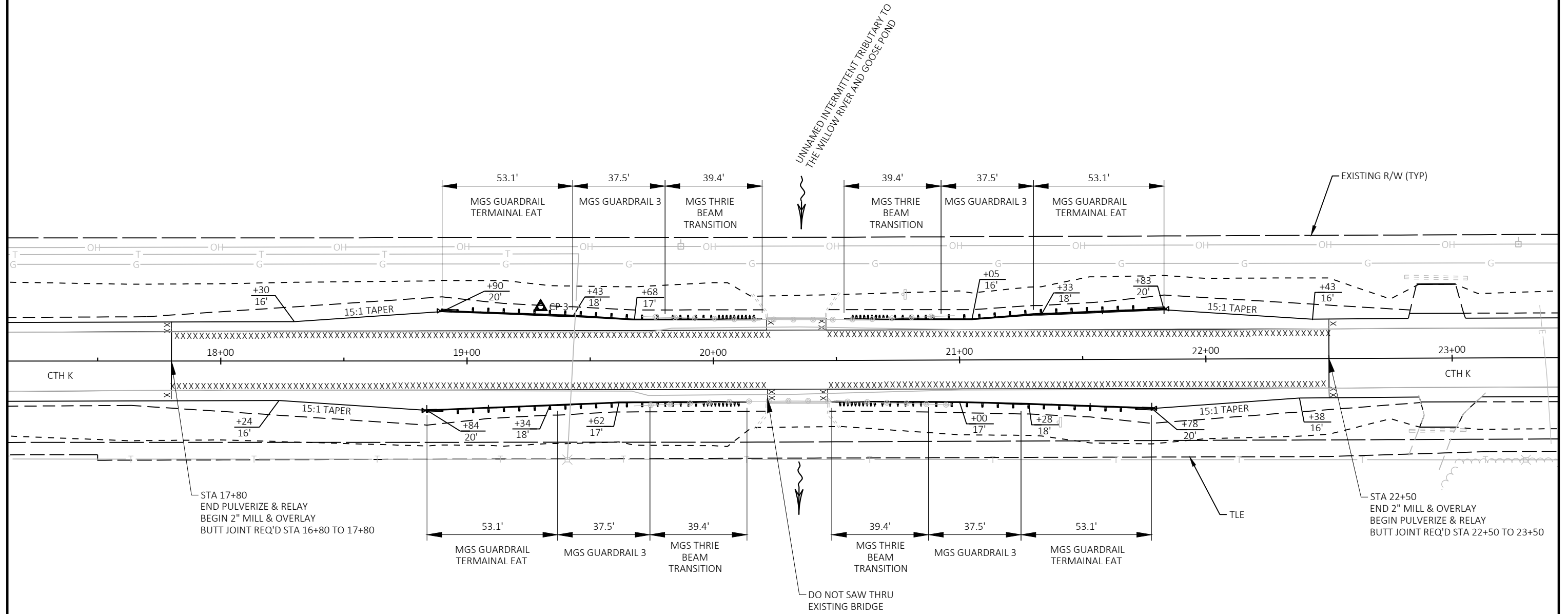


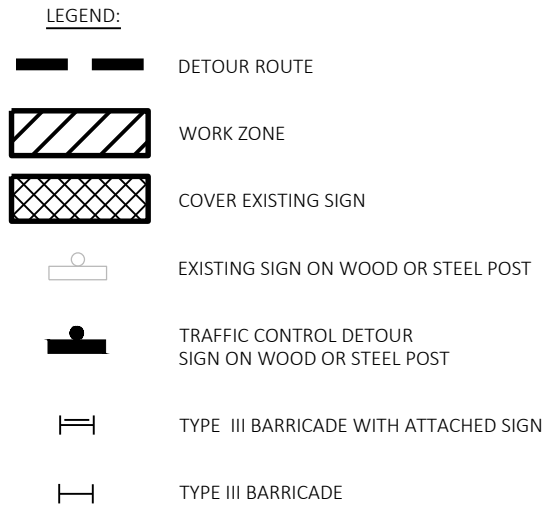
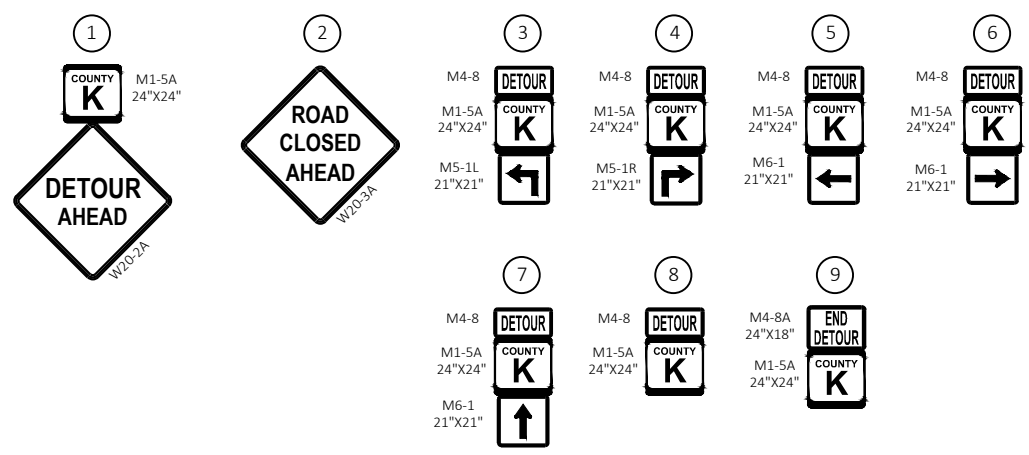
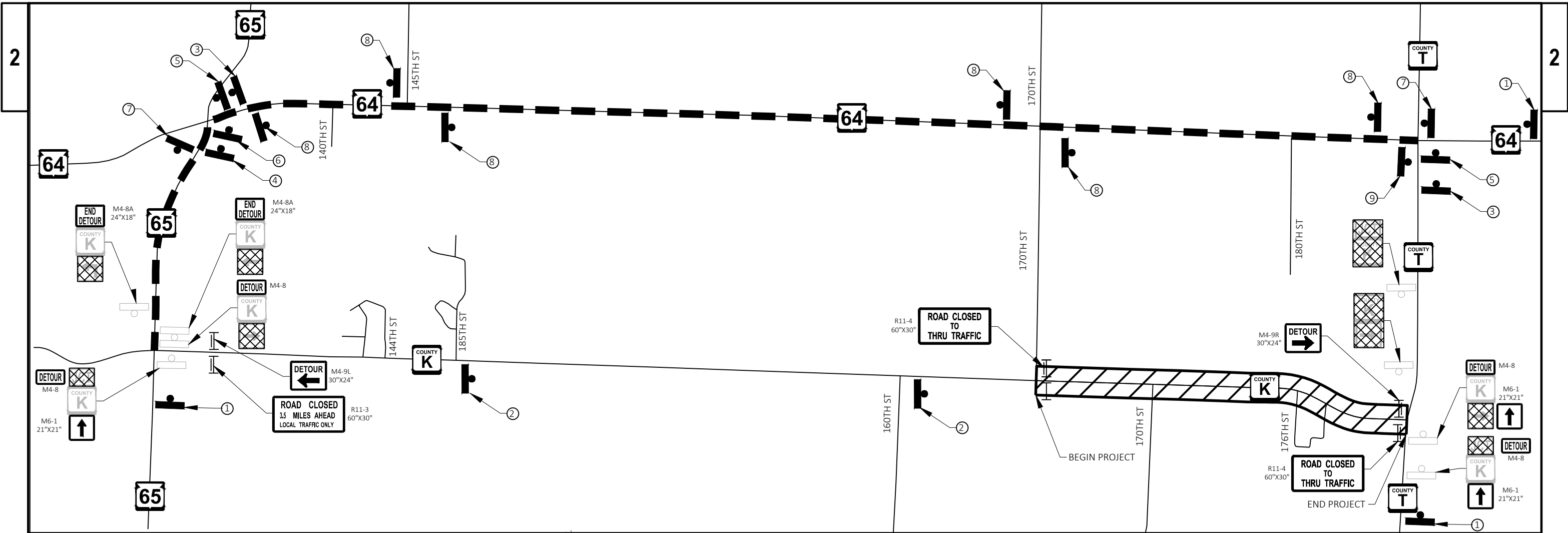
REMOVE MATERIAL UNDER ITEM "REMOVING ASPHALTIC SURFACE BUTT JOINTS" MATERIAL SHALL NOT BE REMOVED UNDER THIS ITEM UNTIL 24 HOURS BEFORE SIDE ROAD PAVING.

SIDE ROAD PAVEMENT DEPTH SHALL MATCH AT MAINLINE PAVEMENT EDGE AND BE TAPERED TO 2" MINIMUM AT JOINT.



SIDE ROAD BUTT JOINT DETAIL
(NOT TO SCALE)





GENERAL NOTES:

DRAWING IS NOT TO SCALE.

THE TRAFFIC CONTROL AND DETOUR ROUTE SHOWN ON THIS SHEET SHALL BE IN PLACE WHEN WHEN CTH K IS CLOSED TO TRAFFIC.

ALL TRAFFIC CONTROL SIGNS AND DEVICES AND THEIR LOCATION SHALL BE IN CONFORMANCE WITH THE WISCONSIN MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD), THE PLANS, STANDARD SPECIFICATIONS AND THE APPLICABLE STANDARD DETAIL DRAWINGS.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS ARE DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

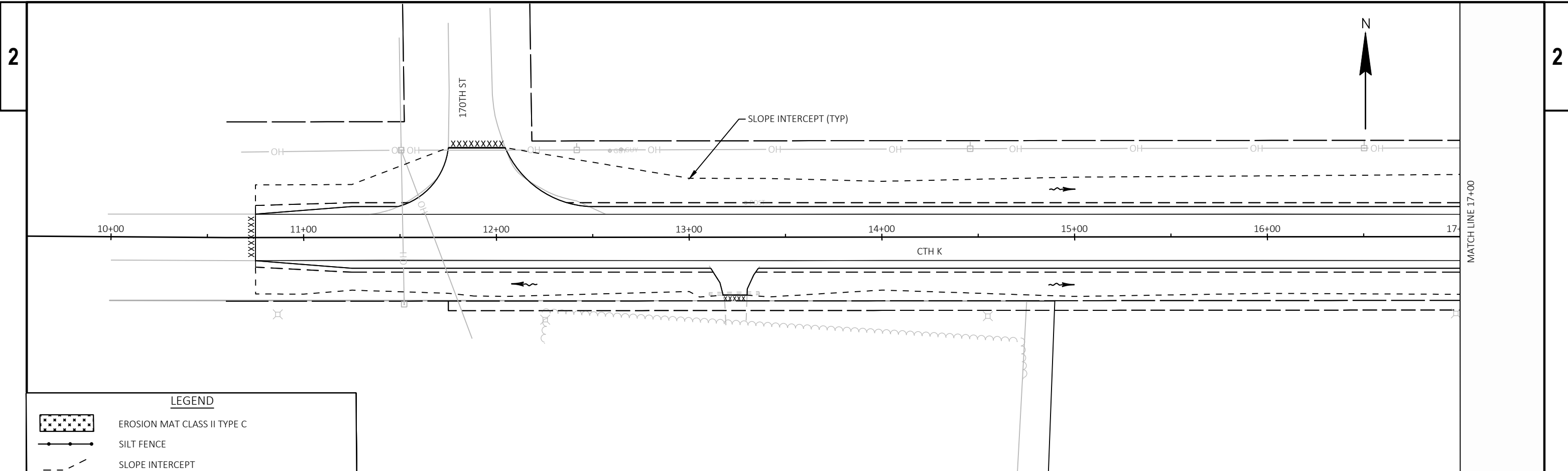
ALL W20 SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE




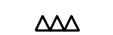


"MO" SIGNS ARE THE SAME AS "M" SIGNS EXCEPT THE BACK GROUND IS ORANGE.

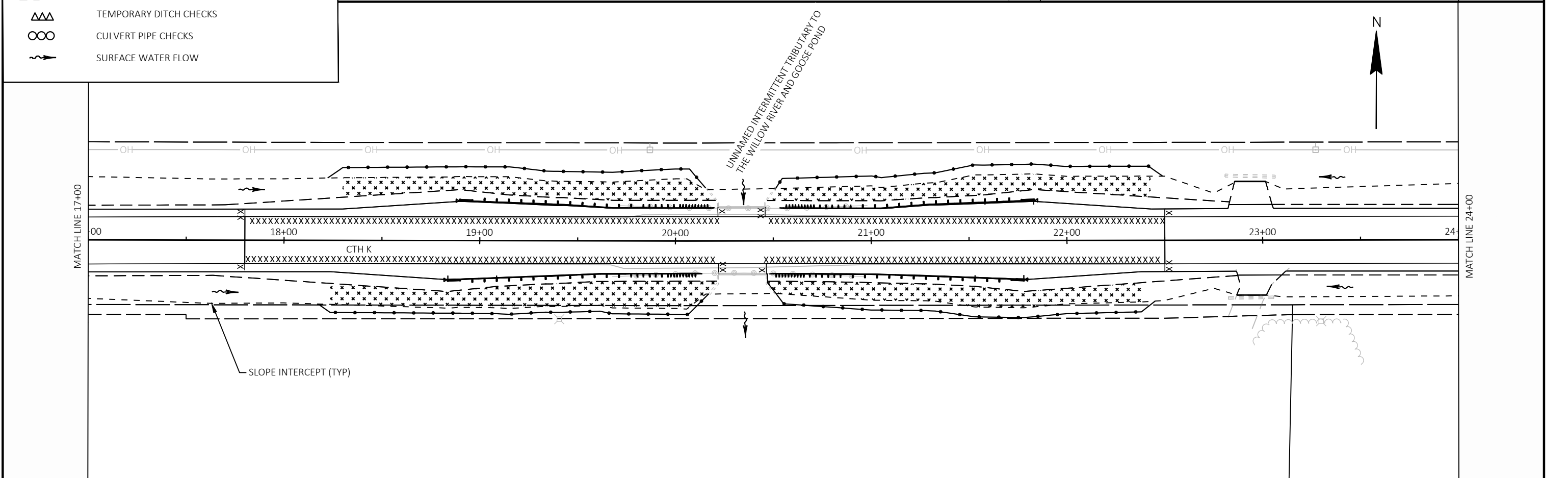
ALL SIGNS INAPPRATE TO THE WORK ZONE, INCLUDING PRE-EXISTING SIGNS, SHALL BE COVERED, REMOVED, OR ALTERED AS SPECIFIED IN THE PLANS AND OR SPECIALS PROVISIONS OR AS DIRECTED BY THE ENGINEER.

ALL SIDEROADS WITHIN THE PROJECT LIMITS SHALL BE BARRICADED AND SIGNED IN ACCORDANCE WITH DETAIL 4 OF THE SDD BARRICADES AND SIGNS FOR SIDEROAD CLOSURES.



LEGEND

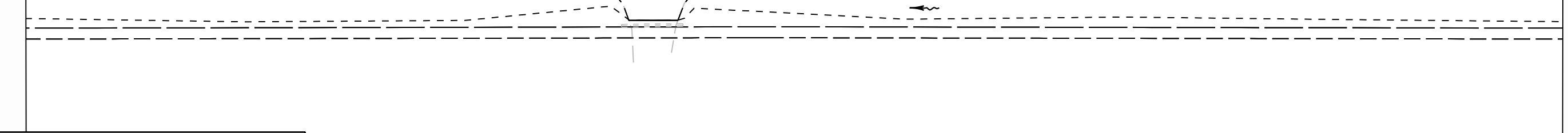
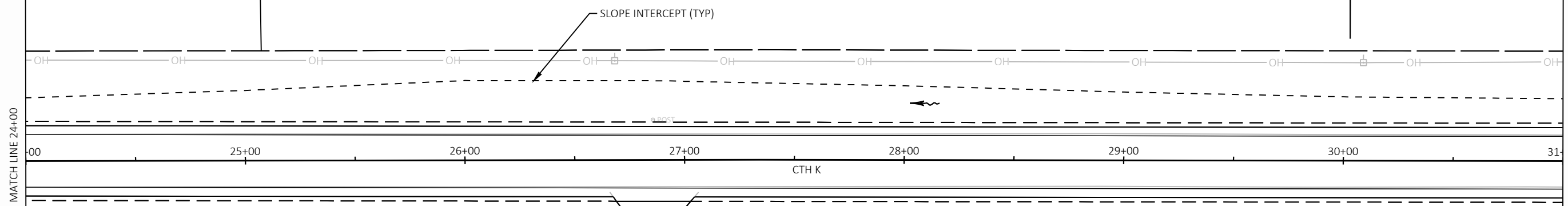
-  EROSION MAT CLASS II TYPE C
-  SILT FENCE
-  SLOPE INTERCEPT
-  TEMPORARY DITCH CHECKS
-  CULVERT PIPE CHECKS
-  SURFACE WATER FLOW




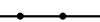




PROJECT NO: 8941-05-71	HWY: CTH K	COUNTY: ST CROIX	EROSION CONTROL	SHEET	E
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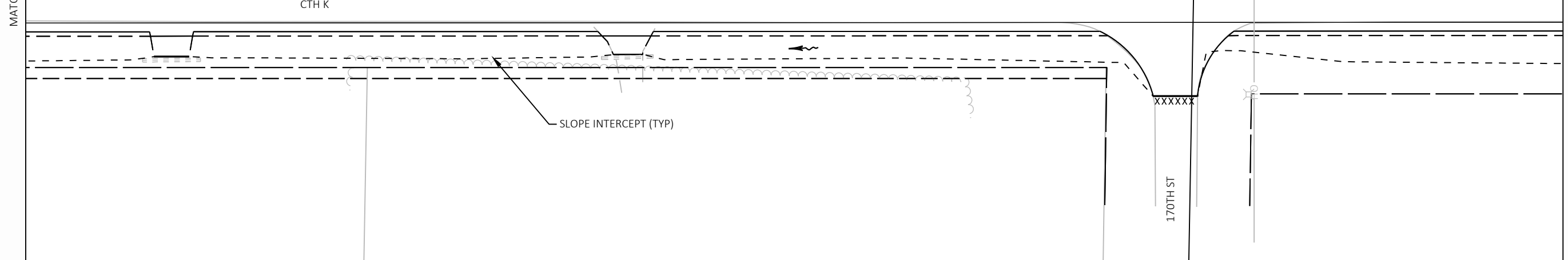
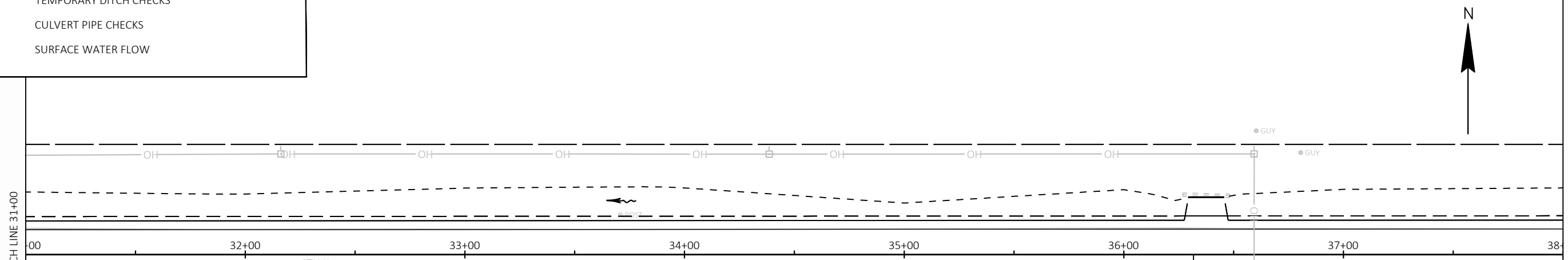
2

2

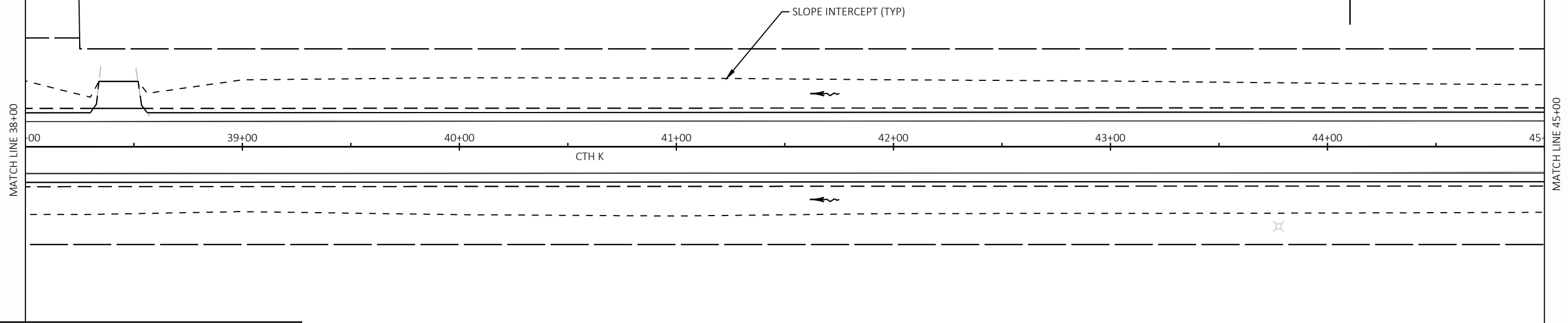


LEGEND







-  EROSION MAT CLASS II TYPE C
-  SILT FENCE
-  SLOPE INTERCEPT
-  TEMPORARY DITCH CHECKS
-  CULVERT PIPE CHECKS
-  SURFACE WATER FLOW

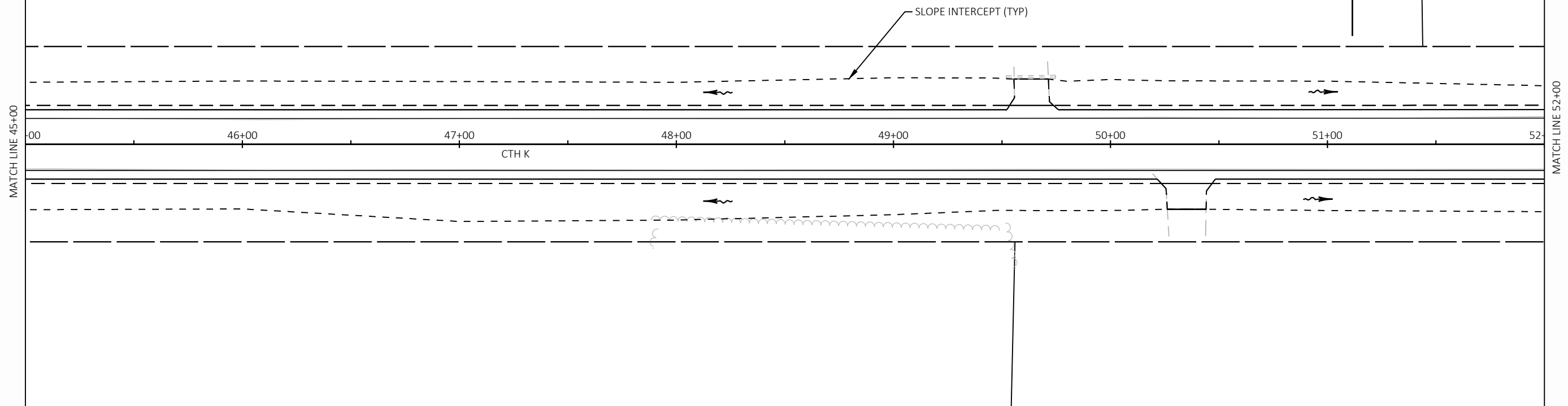


PROJECT NO: 8941-05-71	HWY: CTH K	COUNTY: ST CROIX	EROSION CONTROL	SHEET	E
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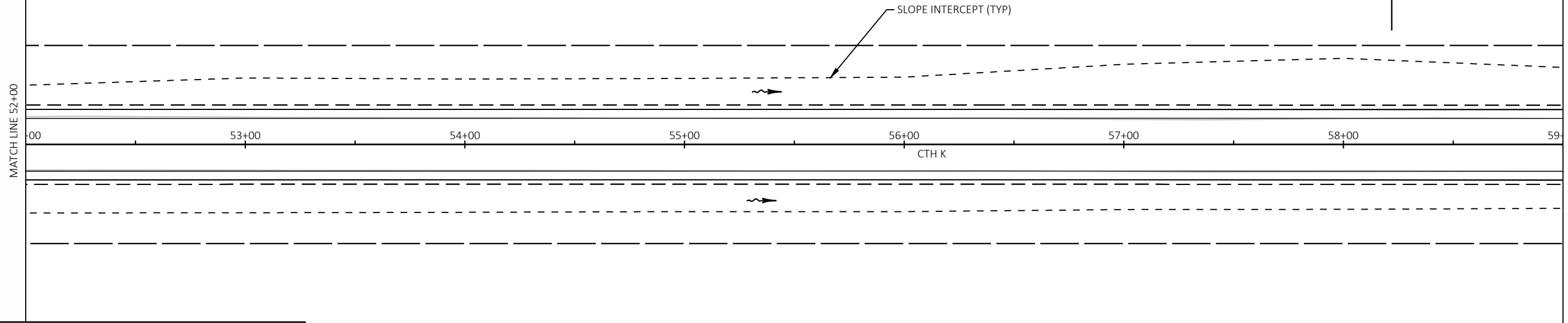


LEGEND


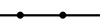

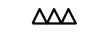

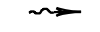
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-  SILT FENCE
-  SLOPE INTERCEPT
-  TEMPORARY DITCH CHECKS
-  CULVERT PIPE CHECKS
-  SURFACE WATER FLOW

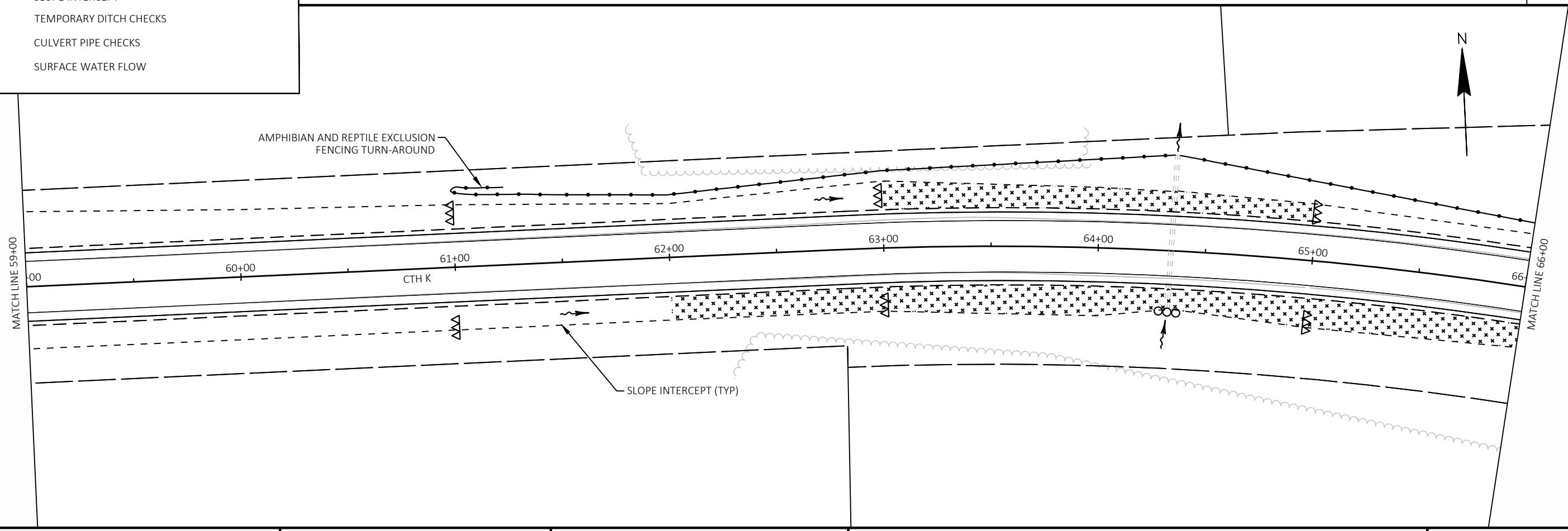


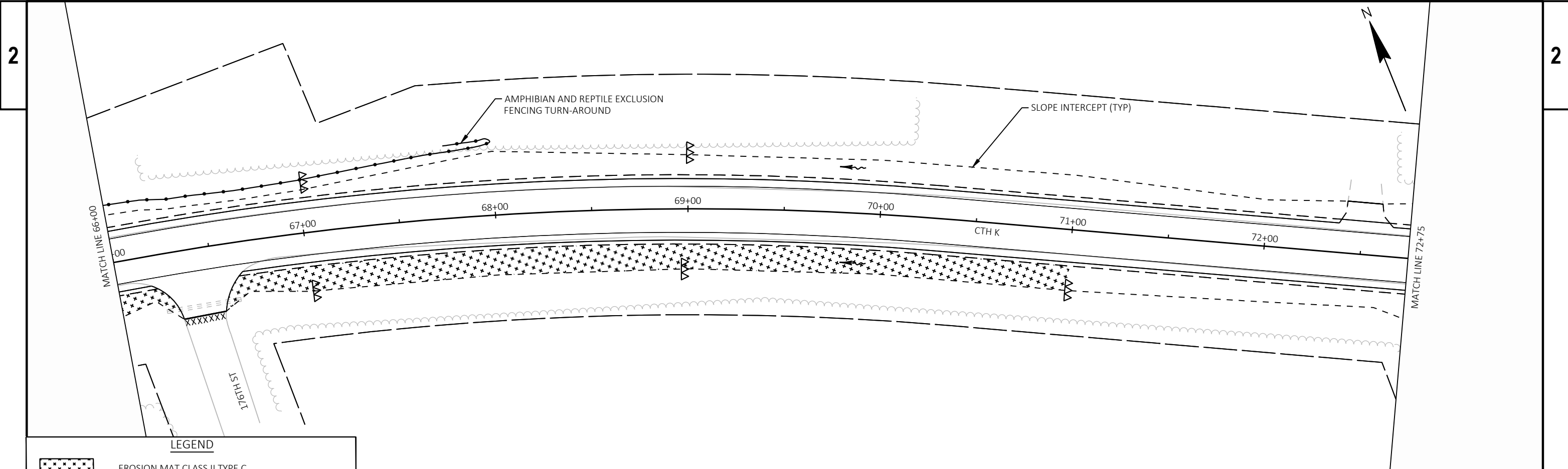
PROJECT NO: 8941-05-71	HWY: CTH K	COUNTY: ST CROIX	EROSION CONTROL	SHEET	E
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LEGEND

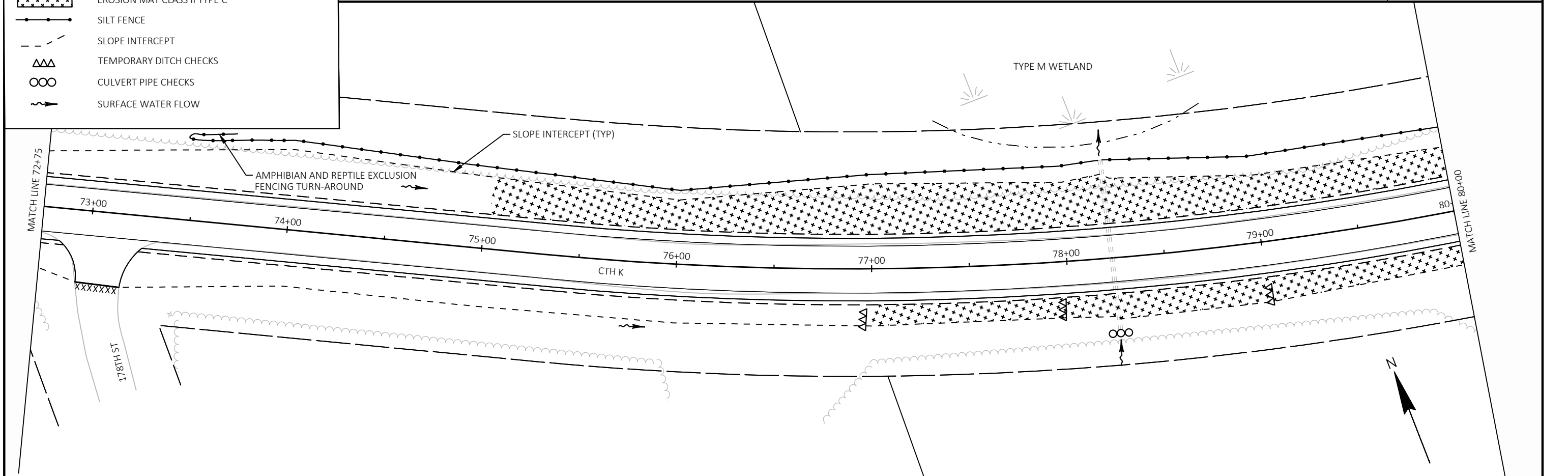
-  EROSION MAT CLASS II TYPE C
-  SILT FENCE
-  SLOPE INTERCEPT
-  TEMPORARY DITCH CHECKS
-  CULVERT PIPE CHECKS
-  SURFACE WATER FLOW



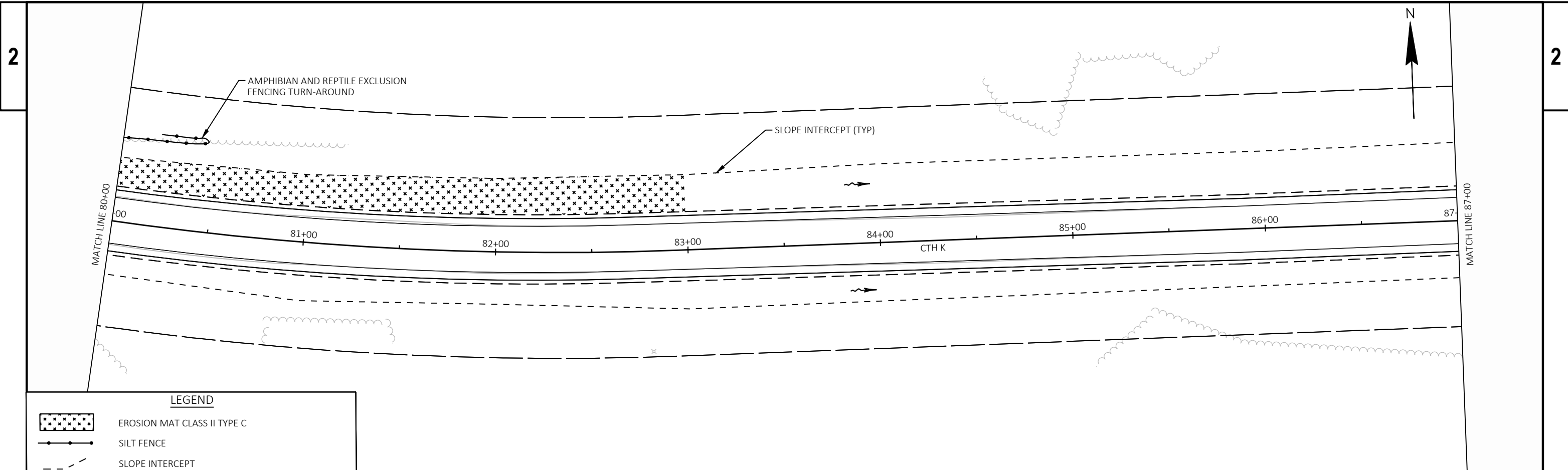


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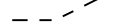
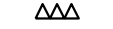


- EROSION MAT CLASS II TYPE C
- SILT FENCE
- SLOPE INTERCEPT
- TEMPORARY DITCH CHECKS
- CULVERT PIPE CHECKS
- SURFACE WATER FLOW

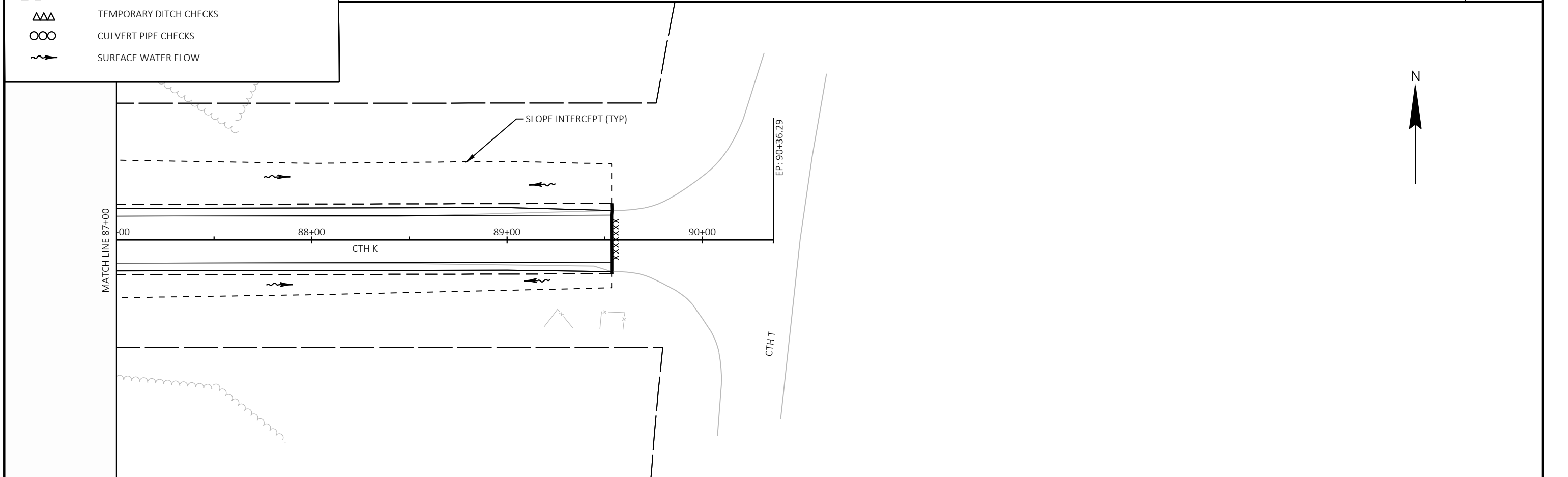


PROJECT NO: 8941-05-71	HWY: CTH K	COUNTY: ST CROIX	EROSION CONTROL
			SHEET E



LEGEND

-  EROSION MAT CLASS II TYPE C
-  SILT FENCE
-  SLOPE INTERCEPT
-  TEMPORARY DITCH CHECKS
-  CULVERT PIPE CHECKS
-  SURFACE WATER FLOW



PROJECT NO: 8941-05-71	HWY: CTH K	COUNTY: ST CROIX	EROSION CONTROL	SHEET E
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Estimate Of Quantities

8941-05-71

Line	Item	Item Description	Unit	Total	Qty
0002	204.0115	Removing Asphaltic Surface Butt Joints	SY	1,346.000	1,346.000
0004	204.0120	Removing Asphaltic Surface Milling	SY	1,285.000	1,285.000
0006	204.0165	Removing Guardrail	LF	181.000	181.000
0008	205.0100	Excavation Common	CY	50.000	50.000
0010	208.0100	Borrow	CY	4,278.000	4,278.000
0012	211.0101	Prepare Foundation for Asphaltic Paving (project) 01. 8941-05-71	EACH	1.000	1.000
0014	213.0100	Finishing Roadway (project) 01. 8941-05-71	EACH	1.000	1.000
0016	305.0110	Base Aggregate Dense 3/4-Inch	TON	1,089.000	1,089.000
0018	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	2,478.000	2,478.000
0020	325.0100	Pulverize and Relay	SY	38,523.000	38,523.000
0022	455.0605	Tack Coat	GAL	3,558.000	3,558.000
0024	460.2000	Incentive Density HMA Pavement	DOL	4,020.000	4,020.000
0026	460.5224	HMA Pavement 4 LT 58-28 S	TON	3,071.000	3,071.000
0028	460.5244	HMA Pavement 4 LT 58-34 S	TON	3,211.000	3,211.000
0030	465.0120	Asphaltic Surface Driveways and Field Entrances	TON	18.000	18.000
0032	520.8700	Cleaning Culvert Pipes	EACH	3.000	3.000
0034	614.2300	MGS Guardrail 3	LF	150.000	150.000
0036	614.2500	MGS Thrie Beam Transition	LF	158.000	158.000
0038	614.2610	MGS Guardrail Terminal EAT	EACH	4.000	4.000
0040	618.0100	Maintenance and Repair of Haul Roads (project) 01. 8941-05-71	EACH	1.000	1.000
0042	619.1000	Mobilization	EACH	1.000	1.000
0044	624.0100	Water	MGAL	420.000	420.000
0046	625.0500	Salvaged Topsoil	SY	24,955.000	24,955.000
0048	627.0200	Mulching	SY	18,583.000	18,583.000
0050	628.1504	Silt Fence	LF	2,319.000	2,319.000
0052	628.1520	Silt Fence Maintenance	LF	2,319.000	2,319.000
0054	628.1905	Mobilizations Erosion Control	EACH	4.000	4.000
0056	628.1910	Mobilizations Emergency Erosion Control	EACH	4.000	4.000
0058	628.2027	Erosion Mat Class II Type C	SY	6,622.000	6,622.000
0060	628.7504	Temporary Ditch Checks	LF	132.000	132.000
0062	628.7555	Culvert Pipe Checks	EACH	2.000	2.000
0064	629.0210	Fertilizer Type B	CWT	15.700	15.700
0066	630.0120	Seeding Mixture No. 20	LB	673.800	673.800
0068	630.0200	Seeding Temporary	LB	673.800	673.800
0070	630.0500	Seed Water	MGAL	700.600	700.600
0072	634.0614	Posts Wood 4x6-Inch X 14-FT	EACH	20.000	20.000
0074	637.2210	Signs Type II Reflective H	SF	59.180	59.180
0076	637.2230	Signs Type II Reflective F	SF	60.240	60.240
0078	638.2102	Moving Signs Type II	EACH	4.000	4.000
0080	638.2602	Removing Signs Type II	EACH	21.000	21.000
0082	638.3000	Removing Small Sign Supports	EACH	20.000	20.000
0084	642.5001	Field Office Type B	EACH	1.000	1.000
0086	643.0300	Traffic Control Drums	DAY	1,152.000	1,152.000
0088	643.0420	Traffic Control Barricades Type III	DAY	1,008.000	1,008.000
0090	643.0705	Traffic Control Warning Lights Type A	DAY	1,152.000	1,152.000
0092	643.0900	Traffic Control Signs	DAY	4,392.000	4,392.000
0094	643.0920	Traffic Control Covering Signs Type II	EACH	10.000	10.000
0096	643.5000	Traffic Control	EACH	1.000	1.000
0098	646.2020	Marking Line Epoxy 6-Inch	LF	26,057.000	26,057.000
0100	648.0100	Locating No-Passing Zones	MI	1.500	1.500

Estimate Of Quantities

8941-05-71

Line	Item	Item Description	Unit	Total	Qty
0102	650.4500	Construction Staking Subgrade	LF	470.000	470.000
0104	650.5000	Construction Staking Base	LF	470.000	470.000
0106	650.8000	Construction Staking Resurfacing Reference	LF	7,878.000	7,878.000
0108	650.9911	Construction Staking Supplemental Control (project) 01. 8941-05-71	EACH	1.000	1.000
0110	650.9920	Construction Staking Slope Stakes	LF	7,878.000	7,878.000
0112	690.0150	Sawing Asphalt	LF	1,095.000	1,095.000
0114	740.0440	Incentive IRI Ride	DOL	5,968.000	5,968.000
0116	ASP.1T0A	On-the-Job Training Apprentice at \$5.00/HR	HRS	300.000	300.000
0118	ASP.1T0G	On-the-Job Training Graduate at \$5.00/HR	HRS	300.000	300.000

3

3

REMOVALS

STATION	LOCATION	204.0115 REMOVING ASPHALTIC SURFACE BUTT JOINTS SY	204.0120 REMOVING ASPHALTIC SURFACE MILLING SY	204.0165 REMOVING GUARDRAIL LF
CTH K				
10+75-11+75	LT & RT	281		
170TH STN	LT & RT	61	-	-
13+25	RT	26	-	-
16+80-17+80	LT & RT	266		
17+80-22+50	LT & RT	-	1285	181
22+50-23+50	LT & RT	267	-	-
170TH ST S	LT & RT	42	-	-
176TH ST	LT & RT	47	-	-
178TH ST	LT & RT	61	-	-
88+53-89+53	LT & RT	296	-	-
ITEM TOTAL		1346	1285	181

EXCAVATION

STATION	LOCATION	205.0100 COMMON CY	AVAILABLE MATERIAL CY	EXPANDED FILL CY	208.0100 BORROW CY
CTH K					
10+75-89+53		-	-	4278	4278
UNDISTRIBUTED		50	-	-	-
ITEM TOTALS		50	0	4278	4278

NOTES:
 1) UNUSABLE PAVEMENT MATERIAL IS INCLUDED IN COMMON EXCAVATION.
 2) AVAILABLE MATERIAL DOES NOT INCLUDE UNUSABLE PAVEMENT EXCAVATION VOLUME.
 3) EXPANSION FACTOR = 1.25

BASE AGGREGATE DENSE

STATION	LOCATION	305.0110 3/4-INCH TON	305.0120 1 1/4-INCH TON	624.0100* WATER MGAL
CTH K				
10+75-17+80	LT & RT	84	209	3
17+80-22+50	LT & RT	115	255	4
22+50-89+53	LT & RT	810	1986	28
AGG DRIVEWAYS	LT & RT	80	-	-
170TH ST N	LT & RT	-	13	-
170TH ST S	LT & RT	-	11	-
176TH ST	LT & RT	-	5	-
ITEM TOTALS		1089	2478	35

*ITEM LOCATED ELSEWHERE IN PLANS

CLEANING CULVERT PIPES

STATION	LOCATION	520.8700 EACH
CTH K		
22+94	LT	1
26+86	RT	1
33+75	RT	1
ITEM TOTAL		3

PULVERIZE AND RELAY

STATION	LOCATION	325.0100 SY	624.0100* WATER MGAL
CTH K			
10+75-17+80	LT&RT	3627	36
22+50-89+53	LT&RT	34486	345
170TH ST N	LT	175	2
170TH ST S	RT	144	1
176TH ST	RT	91	1
ITEM TOTALS		38523	385

*ITEM LOCATED ELSEWHERE IN PLANS

ASPHALTIC PAVEMENT ITEMS

STATION	LOCATION	455.0605 TACK COAT GAL	460.5224 HMA PAVEMENT TON	460.5244 HMA PAVEMENT TON	465.0120 ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES TON
CTH K					
10+75-17+80	LT & RT	310	281	281	-
17+80-22+50	LT & RT	310	62	202	-
22+50-89+53	LT & RT	2870	2670	2670	-
170TH ST N	LT & RT	27	25	25	-
170TH ST S	LT & RT	17	16	16	-
176TH ST	LT & RT	11	10	10	-
178TH ST	LT & RT	8	8	8	-
DRIVEWAYS		5	-	-	18
ITEM TOTALS		3558	3071	3211	18

GUARDRAIL ITEMS

STATION	LOCATION	614.2300 MGS GUARDRAIL 3 LF	614.2500 MGS THRIE BEAM TRANSITION LF	614.2610 MGS GUARDRAIL TERMINAL EAT EACH
CTH K				
18+84-20+21	LT	37.5	39.4	1
18+90-20+21	RT	37.5	39.4	1
20+46-21+78	LT	37.5	39.4	1
20+46-21+83	RT	37.5	39.4	1
ITEM TOTAL		150	158	4

EROSION CONTROL ITEMS

STATION	LOCATION	628.1504 SILT FENCE LF	628.1520 SILT FENCE MAINTENANCE LF	628.2027 EROSION MAT CLASS II TYPE C SY	628.7504 TEMPORARY DITCH CHECKS LF	628.7555 CULVERT PIPE CHECKS EACH
CTH K						
17+00-20+25	LT & RT	405	405	634	48	-
20+45-22+45	LT & RT	402	402	533	36	-
61+00-69+00	LT	665	665	346	30	-
62+00-71+00	RT	-	-	1778	-	1
73+50-83+00	LT	638	638	2513	-	-
77+00-80+00	RT	-	-	568	18	1
UNDISTRIBUTED	LT & RT	210	210	250	-	-
ITEM TOTALS		2319	2319	6622	132	2

MOBILIZATIONS EROSION CONTROL

STATION	628.1905 EROSION CONTROL EACH	628.1910 EMERGENCY EROSION CONTROL EACH
CTH K		
	4	4
ITEM TOTALS		4

3

SALVAGED TOPSOIL, MULCHING AND SEEDING

Table with columns: STATION, LOCATION, SY, SY, CWT, LB, LB, MGAL. Includes item totals for salvaged topsoil, mulching, and seeding.

TRAFFIC CONTROL

Table with columns: STATION, EACH* DAY, EACH* DAY, EACH* DAY, EACH* DAY, EACH, CYCLES, SIGNS, CALENDAR DAYS. Includes item totals for traffic control measures.

*FOR INFORMATION ONLY

PAVEMENT MARKING

Table with columns: STATION, LOCATION, LF, LF, LF, LF, REMARKS. Includes item totals for pavement marking.

PERMANENT SIGNING

Table with columns: STATION, LOCATION, SIGN NUMBER, SIGN CODE, MESSAGE, SIZE, 14-FT EACH, REFLECTIVE H SF, REFLECTIVE F SF, TYPE II EACH, TYPE II EACH, SUPPORTS EACH, REMARKS. Includes item totals for permanent signing.

ITEM TOTALS

LOCATING NO PASSING ZONES

Table with columns: STATION, MI. Shows 1.5 miles for locating no passing zones.

CONSTRUCTION STAKING

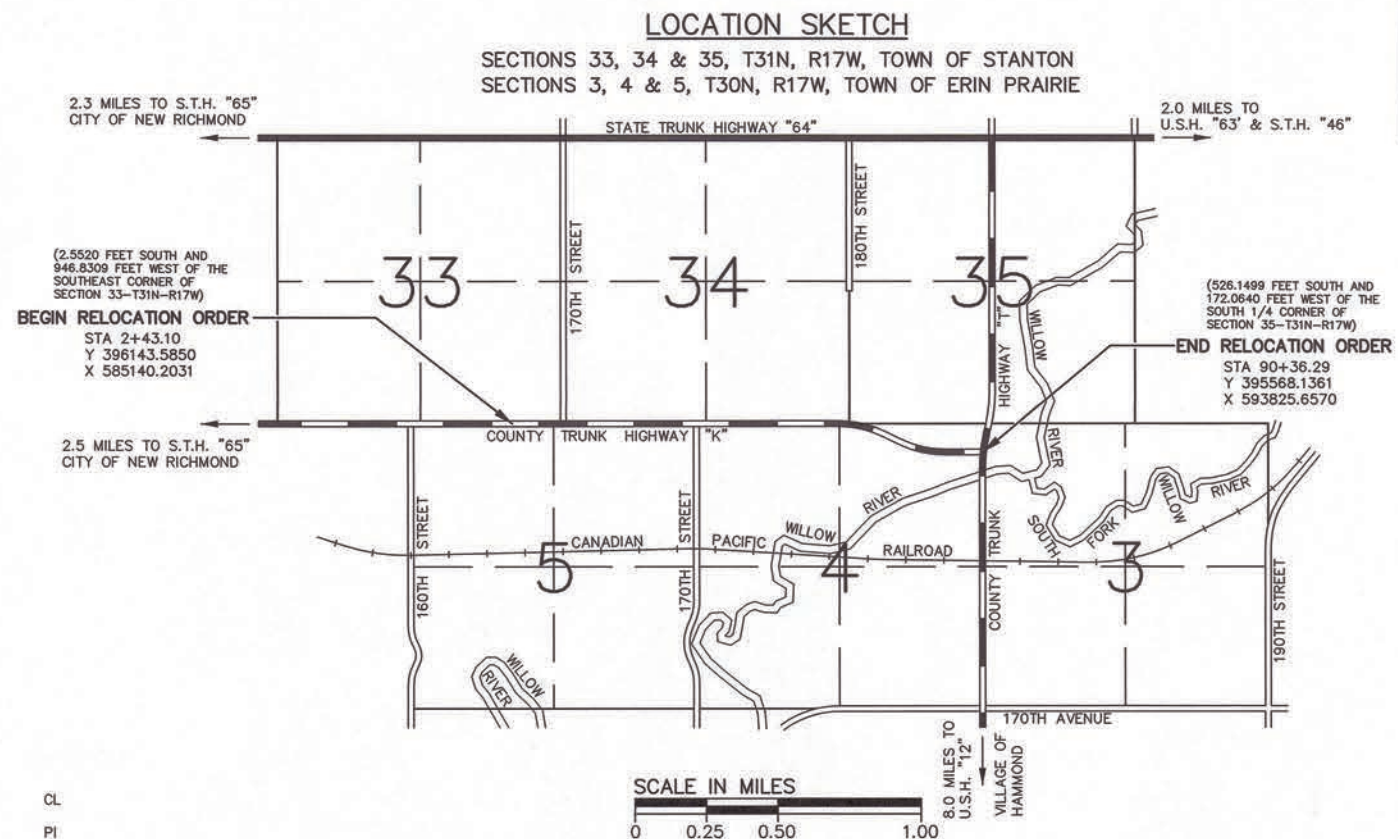
Table with columns: STATION, LF, LF, LF, EACH, LF. Includes item totals for construction staking.

SAWING

Table with columns: STATION, LOCATION, LF. Includes item totals for sawing.

RIGHT-OF-WAY PLAT COUNTY TRUNK HIGHWAY "K" (170TH STREET (NORTH) TO COUNTY TRUNK HIGHWAY "T")

R/W PROJECT NUMBER	SHEET NUMBER	TOTAL SHEETS
8941-05-01	4.01	10
PLAT OF RIGHT-OF-WAY REQUIRED FOR 170TH STREET (NORTH) - C.T.H. "T" TOWN OF STANTON / TOWN OF ERIN PRAIRIE C.T.H. "K" ST. CROIX COUNTY		
DATE: 07-20-2023		



CONVENTIONAL SYMBOLS

PUBLIC LAND SURVEY SYSTEM CORNER FOUND AND MONUMENTED AS LABELED 	DENOTES CENTERLINE CL	DENOTES POINT OF INTERSECTION PI
1-5/16" DIAMETER X 18" IRON PIPE SET, WEIGHING 1.68 LBS. PER LINEAR FOOT 	DENOTES POINT OF CURVATURE PC	DENOTES POINT OF TANGENCY PT
3/4" DIAMETER IRON REBAR FOUND 	DENOTES POINT ON CURVE POC	DENOTES POINT ON TANGENT POT
1-5/16" DIAMETER IRON PIPE FOUND, UNLESS OTHERWISE NOTED 	DENOTES DEGREE OF CURVE (ARC DEFINITION) D	DENOTES RADIUS LENGTH R
1-3/4" DIAMETER IRON PIPE FOUND 	DENOTES CHORD BEARING CB	DENOTES CHORD LENGTH C
ALIGNMENT OR RIGHT-OF-WAY POINT NUMBER (740) 	DENOTES CENTRAL ANGLE A	DENOTES ARC LENGTH A
PREVIOUS HIGHWAY RIGHT-OF-WAY RECORD 	TANGENT BEARING INTO CURVE TAN IN	TANGENT BEARING OUT OF CURVE TAN OUT
QUARTER SECTION OR QUARTER-QUARTER SECTION LINE AS LABELED 	EXISTING ELECTRICAL POWER POLE 	EXISTING GUY ANCHOR
HIGHWAY CENTERLINE ALIGNMENT WITH STATION VALUE 	EXISTING OVERHEAD ELECTRIC LINE 	EXISTING UNDERGROUND ELECTRIC LINE
EXISTING PROPERTY LINE 	EXISTING UNDERGROUND TELEPHONE LINE 	EXISTING UNDERGROUND FIBER-OPTIC LINE
EXISTING HIGHWAY/ROAD RIGHT-OF-WAY 	EXISTING UNDERGROUND GAS LINE 	EXISTING UNDERGROUND TELEPHONE
PROPOSED HIGHWAY/ROAD RIGHT-OF-WAY 	COMPENSABLE UTILITY JUNCTION BOX 	COMPENSABLE UNDERGROUND ELECTRIC
PROPOSED/DESIGNED SLOPE INTERCEPT 	COMPENSABLE UNDERGROUND TELEPHONE 	COMPENSABLE UNDERGROUND GAS
PROPOSED FEE RIGHT-OF-WAY INTEREST (HATCHING VARIES BY LANDOWNER) 		
TEMPORARY LIMITED EASEMENT TLE 		
PROPOSED TEMPORARY LIMITED EASEMENT 		
INDEXED LANDOWNER INTEREST 		
INDEXED UTILITY OWNER INTEREST 		
DENOTES ST. CROIX COUNTY COORDINATE VALUE (Y: NORTHING AND X: EASTING) Y 395856.0000 X 589483.0000		



LENGTH OF PROJECT CENTERLINE= 8,793.19 FEET OR 1.6654 MILES

PLAT NOTES

HORIZONTAL POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN, ST. CROIX COUNTY COORDINATES, NAD83 (1991), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

RIGHT-OF-WAY MONUMENTS FOUND OR PLACED ARE DENOTED IN THE CONVENTIONAL SYMBOLS AND MAPPED OR SHOWN ON THE DETAIL SHEETS OF THIS PLAT. RIGHT-OF-WAY MONUMENTS PLACED, SHALL BE PLACED FOLLOWING THE CONSTRUCTION OF THIS PROJECT.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS, REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM, AS MAPPED OR SHOWN ON THE DETAIL SHEETS OF THIS PLAT.

PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM DATA DERIVED FROM PLATS, MAPS AND DOCUMENTS OF PUBLIC RECORD.

DETERMINATION OF EXISTING HIGHWAY RIGHT-OF-WAY AND ALIGNMENT, AS SHOWN ON THIS PLAT, WAS BASED ON THE FOLLOWING:

- ~ '1972' AND '1973' HIGHWAY CONVEYANCES AND '1972' PLAN DIMENSIONING FROM ST. CROIX COUNTY, HIGHWAY PROJECT 72-K-1(EAST) FOR COUNTY TRUNK HIGHWAY "K".
- ~ '1973' HIGHWAY CONVEYANCE AND '1972' PLAN DIMENSIONING FROM ST. CROIX COUNTY, HIGHWAY PROJECT 72-K-1(WEST) FOR COUNTY TRUNK HIGHWAY "K".
- ~ '1972' QUIT CLAIM DEEDS, '1973' HIGHWAY CONVEYANCE AND '1976' PLAN DIMENSIONING FROM ST. CROIX COUNTY, HIGHWAY PROJECT 76-K-1 FOR COUNTY TRUNK HIGHWAY "K".
- ~ '1978', '1979' AND '1984' QUIT CLAIM DEEDS FROM WILLIAM AND ISABEL WARD, TOGETHER WITH A '1978' MAP OF SURVEY BY RONALD F. JOHNSON FOR SAID WARD, ESTABLISHING COMBINED LAND PARCELS WITHIN THE PLAT OF JEWETT MILLS.
- ~ '1975' HIGHWAY CONVEYANCES AND PLAT FROM STATE OF WISCONSIN - DEPARTMENT OF TRANSPORTATION, PROJECT NUMBER 8964-1-71 FOR COUNTY TRUNK HIGHWAY "T".
- ~ THE COMPILATION OF RECORDED PLATS, RECORDED CERTIFIED SURVEY MAPS AND FILED MAPS OF SURVEY IN ST. CROIX COUNTY.
- ~ VERIFIED WITH THE EXISTING AND TRAVELED CENTERLINES OF BOTH COUNTY TRUNK HIGHWAY "K" AND "T".

A TEMPORARY LIMITED EASEMENT (TLE) IS A RIGHT FOR CONSTRUCTION PURPOSES, AS DEFINED HEREON, INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON AND THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM NECESSARY OR DESIRABLE. ALL TEMPORARY LIMITED EASEMENTS EXPIRE AT THE COMPLETION OF THE CONSTRUCTION PROJECT FOR WHICH THIS INSTRUMENT IS GIVEN.

DIMENSIONING FOR THE NEW RIGHT-OF-WAY IS MEASURED ALONG AND PERPENDICULAR TO THE EXISTING CENTERLINE ALIGNMENT, AS SHOWN ON THIS PLAT.

ACCEPTED FOR ST. CROIX COUNTY

7-20-23
DATE

Robbie Krejci
ROBBIE KREJCI/PE
COUNTY HIGHWAY
COMMISSIONER

PLAT PREPARED BY:

07/20/2023
DATE

Francis W. Bleskacek
FRANCIS W. BLESKACEK, PLS

PREPARED BY:

SURVEYOR ST. CROIX COUNTY
HIGHWAY DEPARTMENT

DESIGNER: SHORT ELLIOTT HENDRICKSON, INC.

REVIEW AUTHORITY: WISCONSIN DEPARTMENT OF TRANSPORTATION
NORTHWEST REGION

REAL ESTATE CONSULTANT: CORRE, INC.

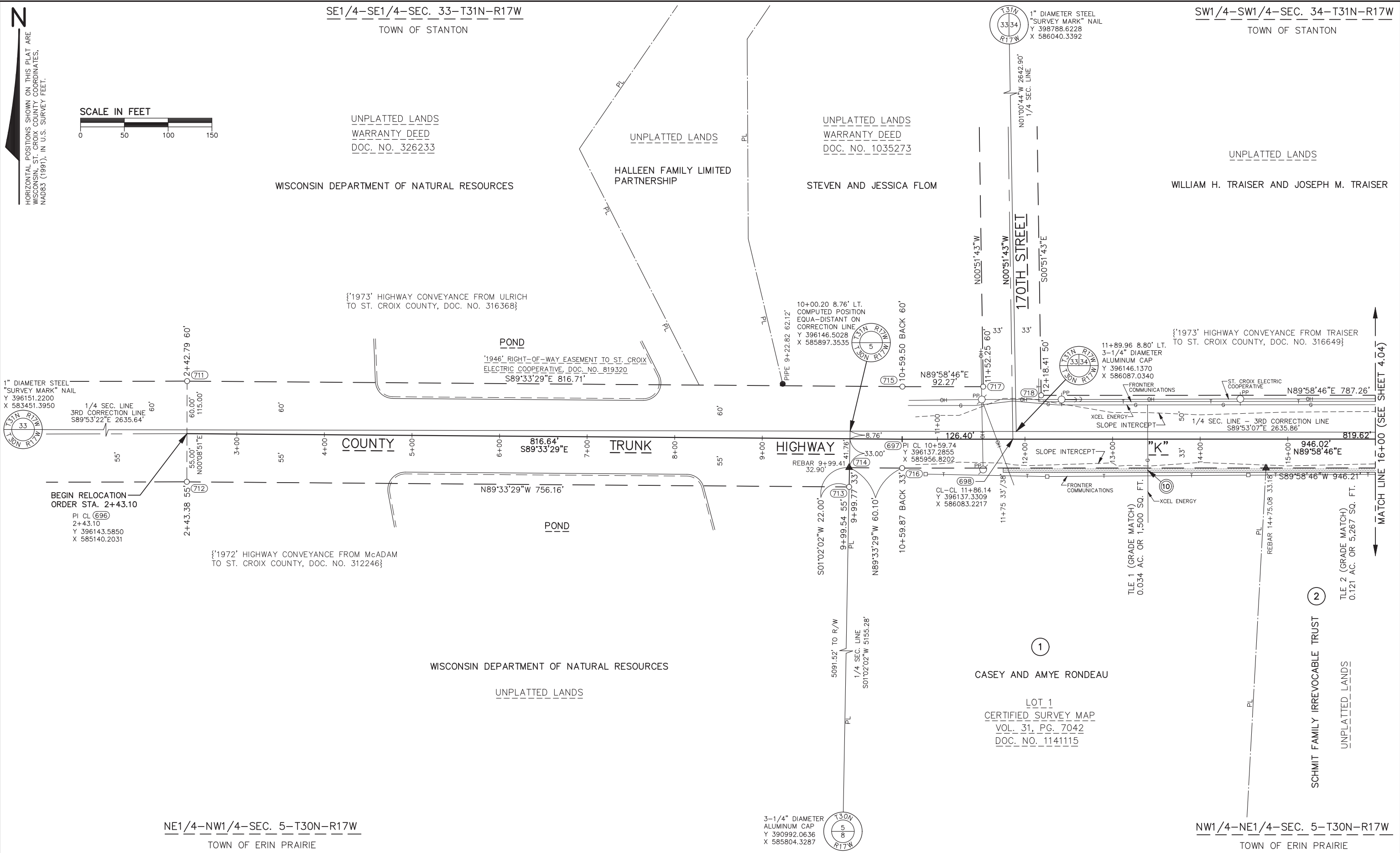
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INTEREST KEY

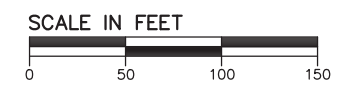
FEE TITLE	FEE
TEMPORARY LIMITED EASEMENT	TLE
CONVEYANCE OF RIGHTS	COR
TEMPORARY CONSTRUCTION EASEMENT	TCE

SCHEDULE OF LANDOWNER INTERESTS							
PARCEL NUMBER	SHEET NUMBER	OWNER(S)	INTEREST REQUIRED	FEE RIGHT-OF-WAY REQUIRED			TEMPORARY LIMITED EASEMENT
				NEW RIGHT-OF-WAY	EXISTING RIGHT-OF-WAY	TOTAL ACQUIRED	
1	4.03	CASEY AND AMYE RONDEAU	TLE				0.034 ACRES OR 1,500 SQ. FT.
2	4.03 & 4.04	SCHMIT FAMILY IRREVOCABLE TRUST	TLE				0.121 ACRES OR 5,267 SQ. FT.
3	4.04 & 4.05	HEATHER ALDERMAN	TLE				0.108 ACRES OR 4,703 SQ. FT.
4	4.05	MICHAEL J. MORIN REVOCABLE LIVING TRUST	TLE				0.039 ACRES OR 1,682 SQ. FT.

SCHEDULE OF UTILITY OWNER INTERESTS			
UTILITY NUMBER	UTILITY OWNER	INTEREST REQUIRED	SHEET NUMBER
10	XCEL ENERGY	TCE	4.03 & 4.05
11	FRONTIER COMMUNICATIONS	TCE	4.04
12	ST. CROIX ELECTRIC COOPERATIVE	TCE	4.04 & 4.05



N
 HORIZONTAL POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN, ST. CROIX COUNTY COORDINATES, NAD83 (1991), IN U.S. SURVEY FEET.



SE1/4-SE1/4-SEC. 33-T31N-R17W
 TOWN OF STANTON

SW1/4-SW1/4-SEC. 34-T31N-R17W
 TOWN OF STANTON

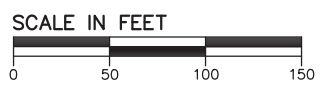
NE1/4-NW1/4-SEC. 5-T30N-R17W
 TOWN OF ERIN PRAIRIE

NW1/4-NE1/4-SEC. 5-T30N-R17W
 TOWN OF ERIN PRAIRIE

N
 HORIZONTAL POSITIONS SHOWN ON THIS PLAT ARE
 WISCONSIN, ST. CROIX COUNTY COORDINATES,
 NAD83 (1991), IN U.S. SURVEY FEET.

SW1/4-SW1/4-SEC. 34-T31N-R17W
 TOWN OF STANTON

SE1/4-SW1/4-SEC. 34-T31N-R17W
 TOWN OF STANTON



UNPLATTED LANDS

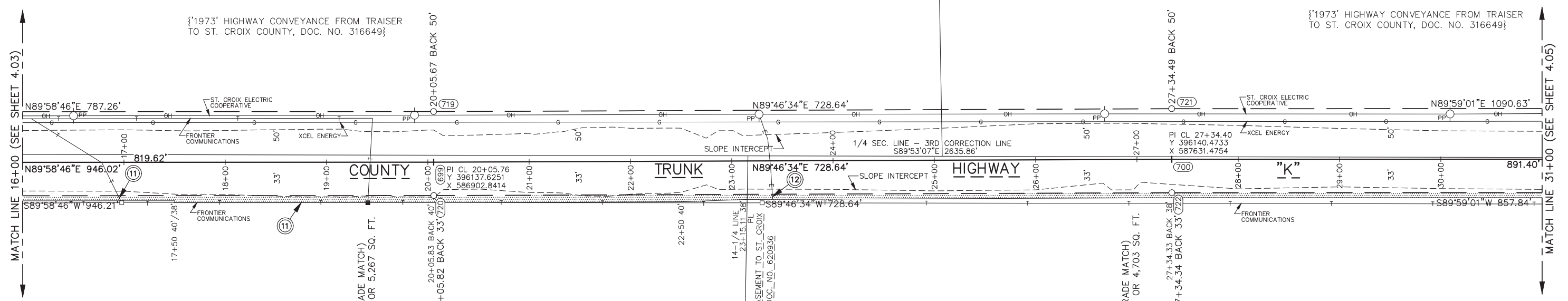
UNPLATTED LANDS

WILLIAM H. TRAISER AND JOSEPH M. TRAISER

WILLIAM H. TRAISER AND JOSEPH M. TRAISER

{1973' HIGHWAY CONVEYANCE FROM TRAISER
 TO ST. CROIX COUNTY, DOC. NO. 316649}

{1973' HIGHWAY CONVEYANCE FROM TRAISER
 TO ST. CROIX COUNTY, DOC. NO. 316649}



SCHMIT FAMILY IRREVOCABLE TRUST

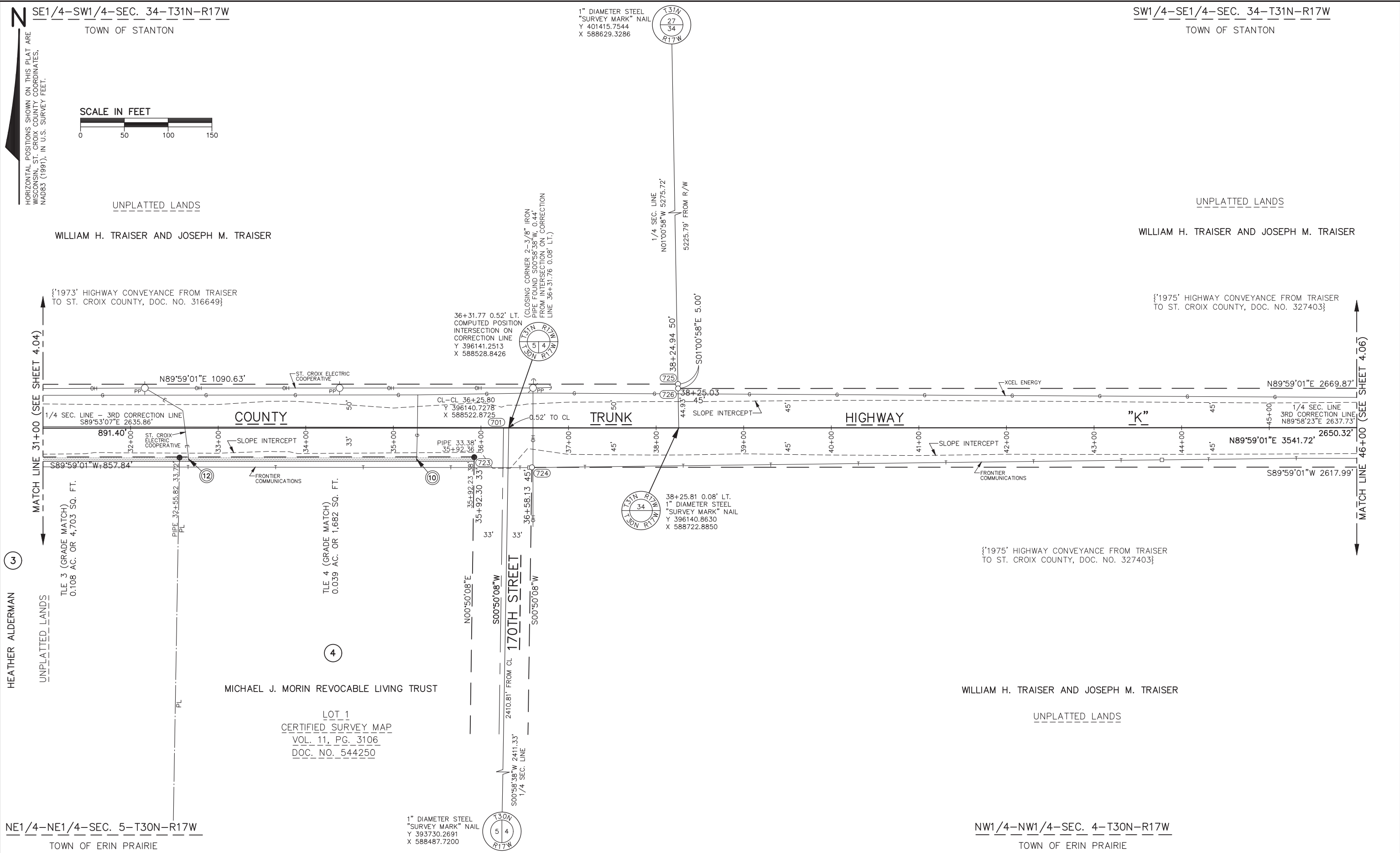
HEATHER ALDERMAN

UNPLATTED LANDS

UNPLATTED LANDS

NW1/4-NE1/4-SEC. 5-T30N-R17W
 TOWN OF ERIN PRAIRIE

NE1/4-NE1/4-SEC. 5-T30N-R17W
 TOWN OF ERIN PRAIRIE



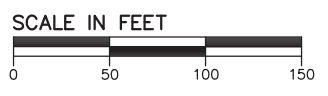
SE1/4-SW1/4-SEC. 34-T31N-R17W

SW1/4-SE1/4-SEC. 34-T31N-R17W

TOWN OF STANTON

TOWN OF STANTON

HORIZONTAL POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN, ST. CROIX COUNTY COORDINATES, NAD83 (1991), IN U.S. SURVEY FEET.



UNPLATTED LANDS

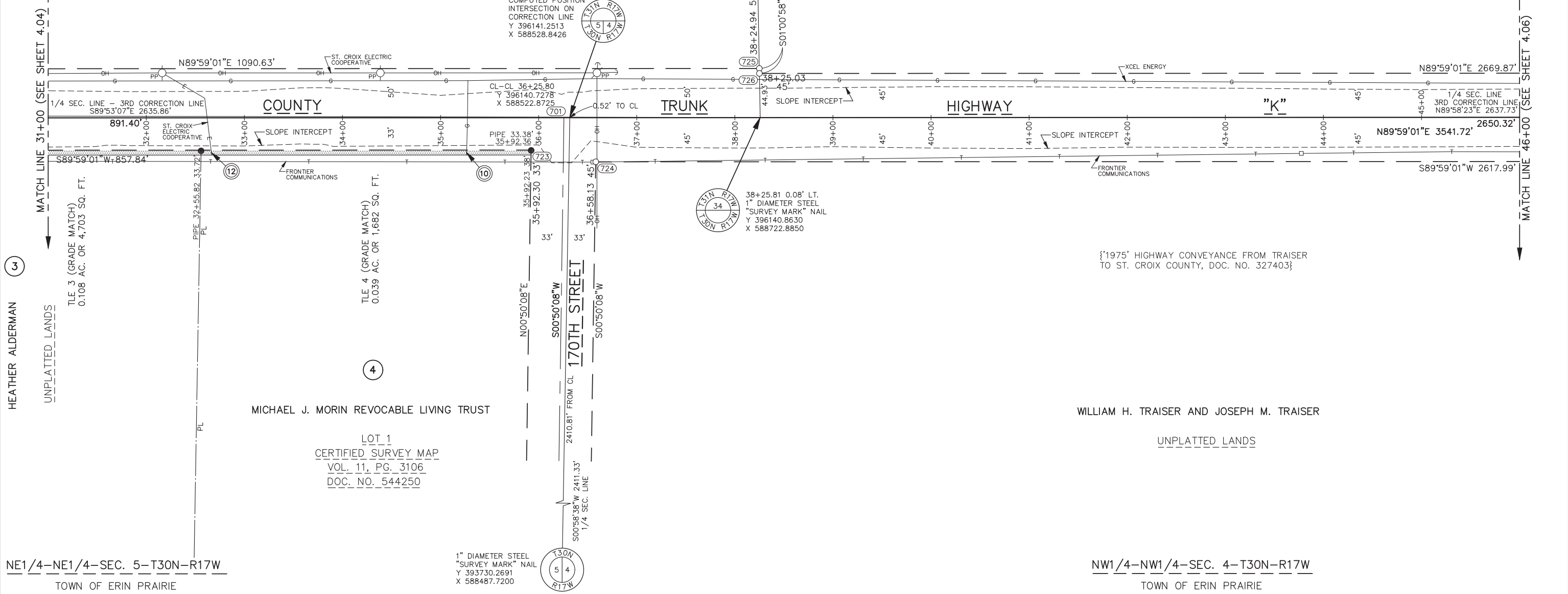
UNPLATTED LANDS

WILLIAM H. TRAISER AND JOSEPH M. TRAISER

WILLIAM H. TRAISER AND JOSEPH M. TRAISER

{1973' HIGHWAY CONVEYANCE FROM TRAISER TO ST. CROIX COUNTY, DOC. NO. 316649}

{1975' HIGHWAY CONVEYANCE FROM TRAISER TO ST. CROIX COUNTY, DOC. NO. 327403}



NE1/4-NE1/4-SEC. 5-T30N-R17W

NW1/4-NW1/4-SEC. 4-T30N-R17W

TOWN OF ERIN PRAIRIE

TOWN OF ERIN PRAIRIE

NW1/4-SE1/4-SEC. 34-T31N-R17W

TOWN OF STANTON

SE1/4-SE1/4-SEC. 34-T31N-R17W

TOWN OF STANTON

HORIZONTAL POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN, ST. CROIX COUNTY COORDINATES, NAD83 (1991), IN U.S. SURVEY FEET.



UNPLATTED LANDS

UNPLATTED LANDS

WILLIAM H. TRAISER AND JOSEPH M. TRAISER

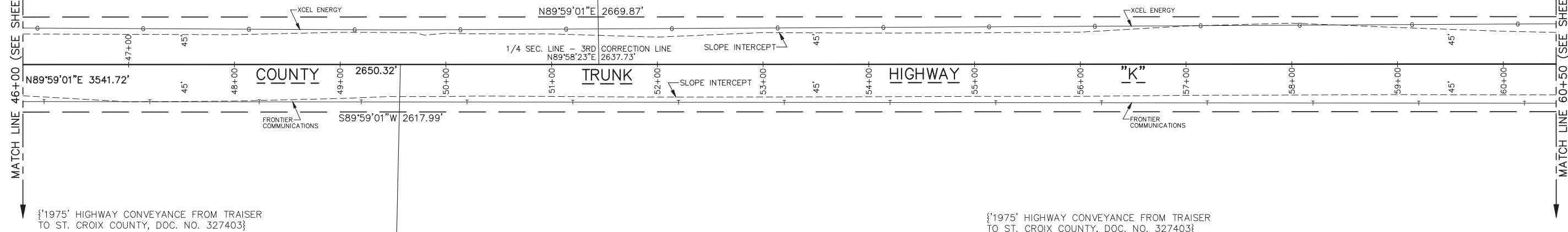
WILLIAM H. TRAISER AND JOSEPH M. TRAISER

{1975' HIGHWAY CONVEYANCE FROM TRAISER TO ST. CROIX COUNTY, DOC. NO. 327403}

{1975' HIGHWAY CONVEYANCE FROM TRAISER TO ST. CROIX COUNTY, DOC. NO. 327403}

MATCH LINE 46+00 (SEE SHEET 4.05)

MATCH LINE 60+50 (SEE SHEET 4.07)



WILLIAM H. TRAISER AND JOSEPH M. TRAISER

WILLIAM H. TRAISER AND JOSEPH M. TRAISER

UNPLATTED LANDS

UNPLATTED LANDS

NW1/4-NW1/4-SEC. 4-T30N-R17W

TOWN OF ERIN PRAIRIE

NE1/4-NW1/4-SEC. 4-T30N-R17W

TOWN OF ERIN PRAIRIE

TOWN OF STANTON

TOWN OF STANTON

UNPLATTED LANDS

UNPLATTED LANDS

WILLIAM H. TRAISER AND JOSEPH M. TRAISER

WILLIAM H. TRAISER AND JOSEPH M. TRAISER

SCALE IN FEET



N
HORIZONTAL POSITIONS SHOWN ON THIS PLAT ARE
WISCONSIN, ST. CROIX COUNTY COORDINATES,
NAD83 (1991), IN U.S. SURVEY FEET.

{1975' HIGHWAY CONVEYANCE FROM TRAISER
TO ST. CROIX COUNTY, DOC. NO. 327403}

{1975' HIGHWAY CONVEYANCE FROM TRAISER
TO ST. CROIX COUNTY, DOC. NO. 327403}

64+62.66 11.26' LT.
1" DIAMETER STEEL
"SURVEY MARK" NAIL
Y 396142.1080
X 591360.6170



3-1/4" DIAMETER
ALUMINUM CAP
Y 396094.2860
X 593997.7210

MATCH LINE 60+50 (SEE SHEET 4.06)

1/4 SEC. LINE - 3RD CORRECTION LINE
588°57'40"E 2637.54'
2323.80' FROM R/W

N89°59'01"E 2669.87'
PC CL 62+76.12
Y 396141.4846
X 591173.1932

258.93'
POND
S88°57'40"E 227.07'
67+01.26 99.03'
45.02'
S00°49'55"E

CURVE 735-736:
R= 1707.022'
Δ= 9°43'28"
CB= S68°04'59"E
C= 289.38'
A= 289.73'

2650.32'
N89°59'01"E 3541.72'
S89°59'01"W 2617.99'

ARC= 354.17'
CL-CL 66+30.29
Y 396103.4228
X 591524.6130

PART OF: BLOCK "73", BLOCK "74", PLAT OF
JEWETT MILLS AND UNPLATTED LANDS

CURVE 728-727:
R= 1592.022'
Δ= 0°10'39"
CB= N89°55'39.5"W
C= 4.93'
A= 4.93'

ARC= 411.42'
SLOPE INTERCEPT
R/W PER '1978' WARD SURVEY

'1978' RIGHT-OF-WAY EASEMENT TO ST. CROIX
ELECTRIC COOPERATIVE, DOC. NO. 349502
THOMAS AND RITA HIAM
PART OF: BLOCK "75" AND VACATED
GRANT STREET, PLAT OF JEWETT MILLS

{1975' HIGHWAY CONVEYANCE FROM TRAISER
TO ST. CROIX COUNTY, DOC. NO. 327403}

CURVE 731-729:
R= 1582.022'
Δ= 11°24'34"
CB= N84°08'35"W
C= 314.51'
A= 315.03'

DANIEL AND DANA NORK
PART OF BLOCK "74"
PLAT OF JEWETT MILLS

WILLIAM H. TRAISER AND JOSEPH M. TRAISER

NICHOLAS CLOBES AND
KATHRYN TERRELL

UNPLATTED LANDS

PART OF: BLOCK "73" AND VACATED
CLARK STREET, PLAT OF JEWETT MILLS

1/4 SEC. LINE
S01°33'50"W 5001.66'
4946.10' FROM R/W

WARRANTY DEED FROM WARD TO CUNNINGHAM, LYING
SOUTH OF HIGHWAY AS NOW LAYED OUT AND TRAVELED,
DOC. NO. 360212; AS SHOWN ON '1976' HIGHWAY PLAT}

CURVE 737-732:
R= 1582.022'
Δ= 12°46'33"
CB= N69°36'31.5"W
C= 352.03'
A= 352.76'

{1978' WARRANTY DEED FROM WARD TO PETERS, LYING
SOUTH OF HIGHWAY AS NOW LAYED OUT AND TRAVELED,
DOC. NO. 348687; AS SHOWN ON '1976' HIGHWAY PLAT}

{1978' WARRANTY DEED FROM WARD TO WARD, LYING
SOUTH OF HIGHWAY AS NOW LAYED OUT AND TRAVELED,
DOC. NO. 351017; AS SHOWN ON '1976' HIGHWAY PLAT}

TOWN OF ERIN PRAIRIE

TOWN OF ERIN PRAIRIE

1" DIAMETER STEEL
"SURVEY MARK" NAIL
Y 391142.2207
X 591042.8851

CURVE 702-704:
D= 3'30'00"
R= 1637.022'
Δ= 26°47'44"
CB= S76°37'07"E
C= 758.63'
A= 765.59'
T= 389.93'
TAN IN= N89°59'01"E
TAN OUT S63°13'15"E
RAD PT: Y 394504.4627
X 591173.6606

MARGARET AND PATRICK MOORE
PART OF: BLOCK "71", BLOCK "72", BLOCK "79",
BLOCK "80", VACATED WABASHAW STREET,
VACATED JEFFERSON STREET AND VACATED
SECOND STREET NORTH, PLAT OF JEWETT MILLS

{1984' WARRANTY DEED FROM WARD TO MOORE, LYING
SOUTH OF HIGHWAY AS NOW LAYED OUT AND TRAVELED,
DOC. NO. 391645; AS SHOWN ON '1976' HIGHWAY PLAT}

PART OF: BLOCK "73",
BLOCK "74", PLAT OF JEWETT
MILLS AND UNPLATTED LANDS

ST. CROIX COUNTY

{1972' QUIT CLAIM DEED
FROM WARD TO ST. CROIX
COUNTY, DOC. NO. 311329}

CURVE 706-707:
D= 3'30'00"
R= 1637.022'
Δ= 26°41'03"
CB= S76°33'46.5"E
C= 755.54'
A= 762.41'
T= 388.25'
TAN IN= S63°13'15"E
TAN OUT S89°54'18"E
RAD PT: Y 397206.3842
X 593086.6508

PART OF: BLOCK "68", BLOCK "69", BLOCK "70", BLOCK "71",
PLAT OF JEWETT MILLS AND UNPLATTED LANDS

ST. CROIX COUNTY

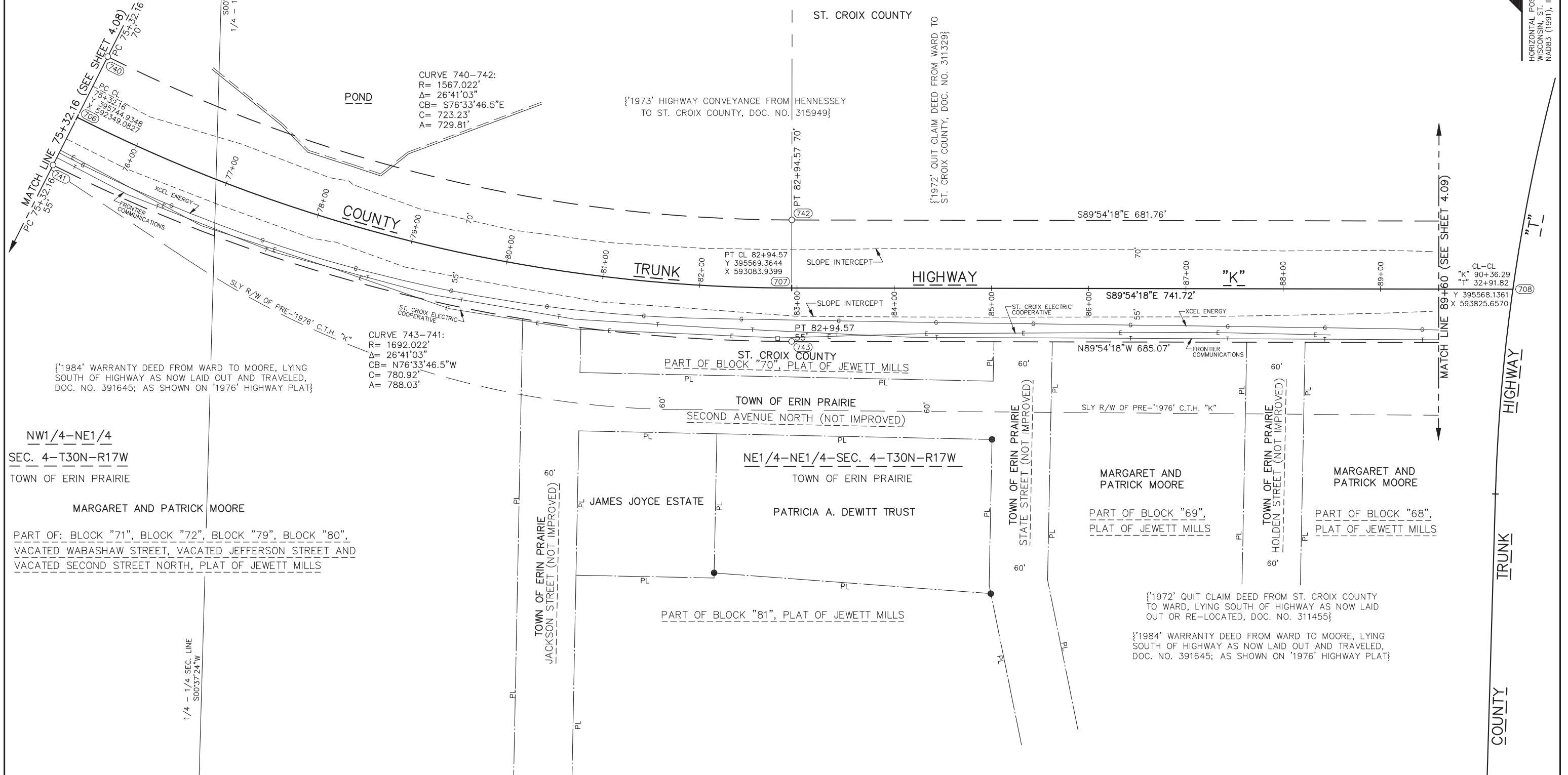
{1973' HIGHWAY CONVEYANCE FROM HENNESSEY
TO ST. CROIX COUNTY, DOC. NO. 315949}

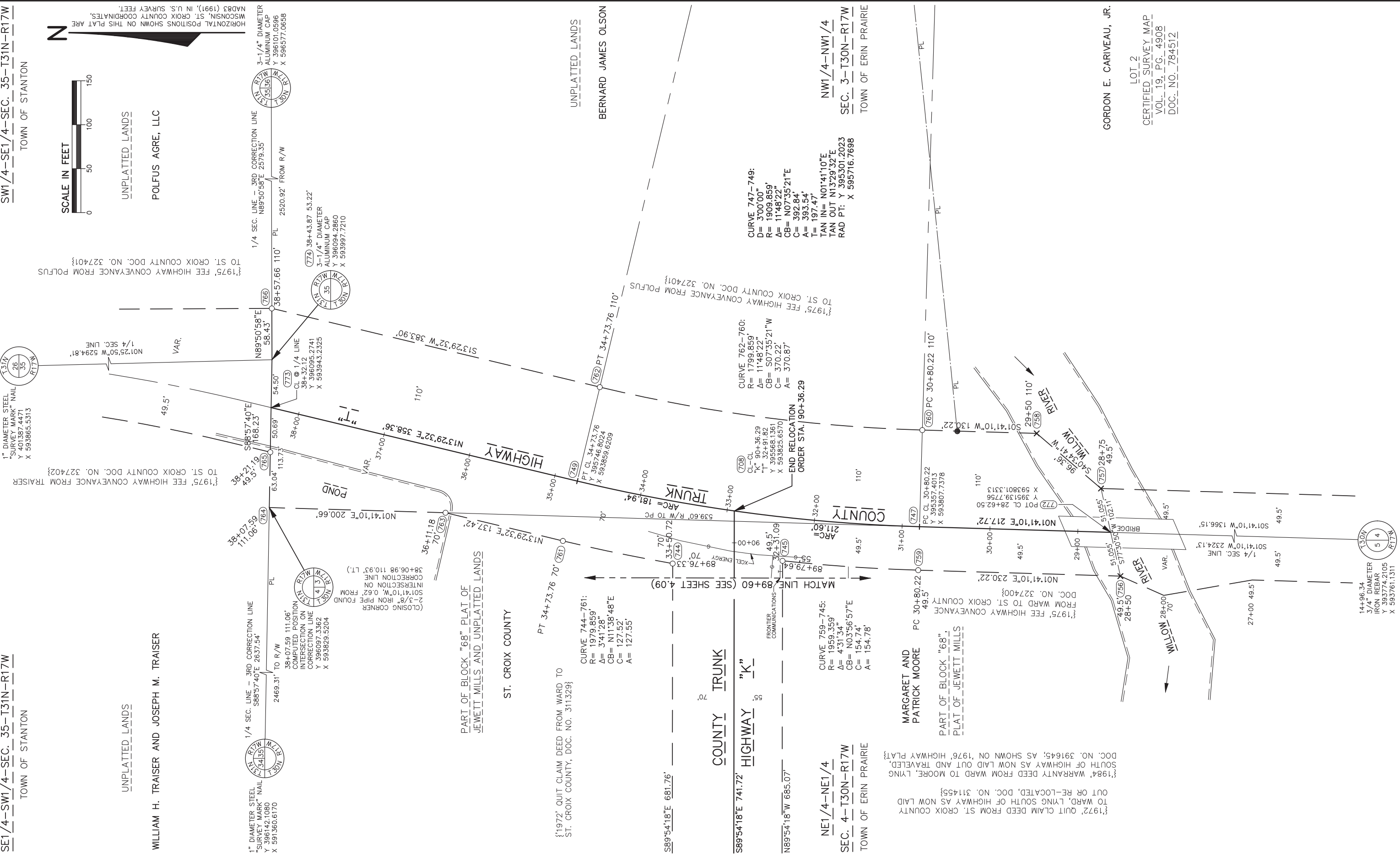
{1972' QUIT CLAIM DEED FROM WARD TO
ST. CROIX COUNTY, DOC. NO. 311329}

SCALE IN FEET



N
HORIZONTAL POSITIONS SHOWN ON THIS PLAT ARE
WISCONSIN ST. CROIX COUNTY COORDINATES,
NAD83 (1991), IN U.S. SURVEY FEET.





CENTERLINE COUNTY TRUNK HIGHWAY "K" ALIGNMENT

POINT NO.	STATION	Y COORDINATE	X COORDINATE	DESCRIPTION
696	2+43.10	396143.5850	585140.2031	PI CL
S89°33'29"E 816.64'				
697	10+59.74	396137.2855	585956.8202	PI CL
N89°58'46"E 126.40'				
698	11+86.14	396137.3309	586083.2217	CL-CL
N89°58'46"E 819.62'				
699	20+05.76	396137.6251	586902.8414	PI CL
N89°46'34"E 728.64'				
700	27+34.40	396140.4733	587631.4754	PI CL
N89°59'01"E 891.40'				
701	36+25.80	396140.7278	588522.8725	CL-CL
N89°59'01"E 2650.32'				
702	62+76.12	396141.4846	591173.1932	PC CL
CURVE: R= 1637.022' Δ= 12°23'45" CB= S83°49'06.5"E C= 353.48' A= 354.17'				
703	66+30.29	396103.4228	591524.6130	CL-CL
CURVE: R= 1637.022' Δ= 14°23'59" CB= S70°25'14.5"E C= 410.34' A= 411.42'				
704	70+41.71	395965.9121	591911.2288	PT CL
S63°13'15"E 234.57'				
705	72+76.28	395860.2265	592120.6387	CL-CL
S63°13'15"E 255.88'				
706	75+32.16	395744.9348	592349.0827	PC CL
CURVE: R= 1637.022' Δ= 26°41'03" CB= S76°33'46.5"E C= 755.54' A= 762.41'				
707	82+94.57	395569.3644	593083.9399	PT CL
S89°54'18"E 741.72'				
708	90+36.29	395568.1361	593825.6570	CL-CL

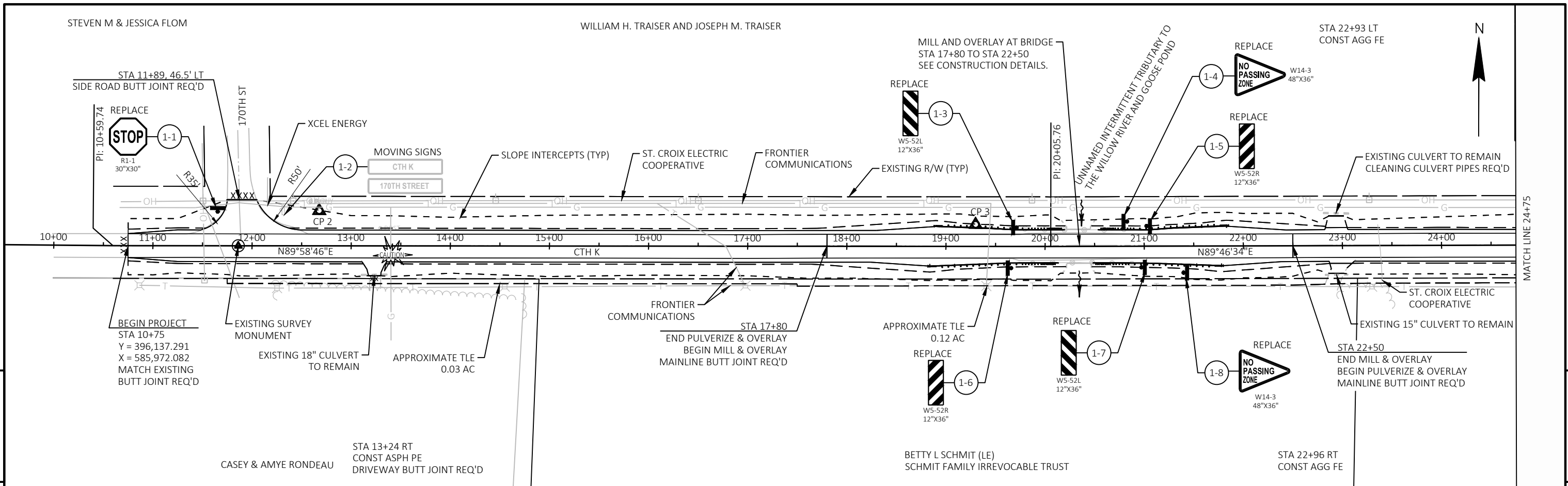
CENTERLINE COUNTY TRUNK HIGHWAY "T" ALIGNMENT

POINT NO.	STATION	Y COORDINATE	X COORDINATE	DESCRIPTION
772	28+62.50	395139.7756	593801.3313	POT CL
N01°41'10"E 217.72'				
747	30+80.22	395357.4013	593807.7378	PC CL
CURVE: R= 1909.859' Δ= 6°20'53" CB= N04°51'36.5"E C= 211.49' A= 211.60'				
708	32+91.82	395568.1361	593825.6570	CL-CL
CURVE: R= 1909.859' Δ= 5°27'29" CB= N10°45'47.5"E C= 181.87' A= 181.94'				
749	34+73.76	395746.8024	593859.6209	PT CL
N13°29'32"E 358.36'				
773	38+32.12	396095.2741	593943.2325	POT CL

RIGHT-OF-WAY COURSE TABLE

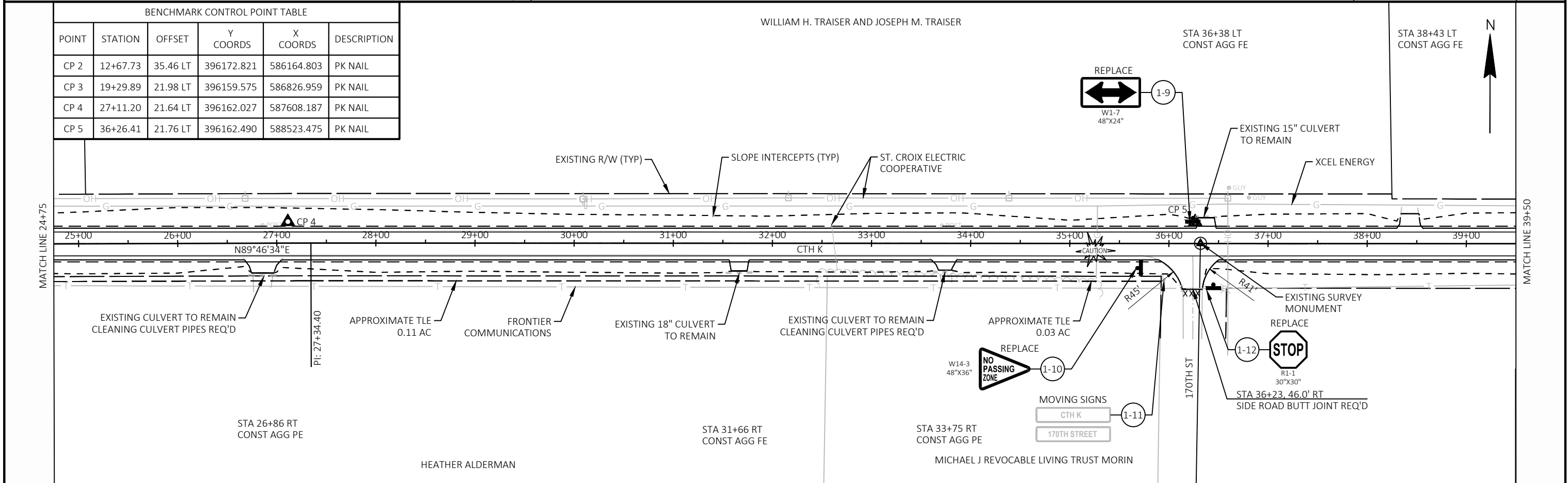
POINT NO.	STATION	OFFSET	Y COORDINATE	X COORDINATE
696	2+43.10	ON CL	396143.5850	585140.2031
N00°08'51"E 60.00'				
711	2+42.79	60' LT.	396203.5856	585140.3576
S89°33'29"E 816.71'				
715	10+59.50	60' LT.	396197.2856	585957.0409
N89°58'46"E 92.27'				
717	11+52.25	60' LT.	396197.3187	586049.3153
S81°25'29"E 66.91'				
718	12+18.41	50' LT.	396187.3425	586115.4729
N89°58'46"E 787.26'				
719	20+05.67	50' LT.	396187.6251	586902.7347
N89°46'34"E 728.64'				
721	27+34.49	50' LT.	396190.4732	587631.3705
N89°59'01"E 1090.63'				
725	38+24.94	50' LT.	396190.7847	588721.9996
S01°00'58"E 5.00'				
726	38+25.03	45' LT.	396185.7847	588722.0883
N89°59'01"E 2669.87'				
730	64+87.85	59.17' LT.	396186.5471	591391.9559
S88°57'40"E 227.07'				
733	67+01.26	99.03' LT.	396182.4300	591618.9888
S00°49'55"E 45.02'				
734	67+13.04	55.73' LT.	396137.4108	591619.6425
S88°57'40"E 54.67'				
735	67+63.86	70' LT.	396136.4196	591674.3011
CURVE: R= 1707.022' Δ= 9°43'28" CB= S68°04'59"E C= 289.38' A= 289.73'				
736	70+41.71	70' LT.	396028.4045	591942.7676
S63°13'15"E 490.45'				
740	75+32.16	70' LT.	395807.4272	592380.6215
CURVE: R= 1567.022' Δ= 26°41'03" CB= S76°33'46.5"E C= 723.23' A= 729.81'				
742	82+94.57	70' LT.	395639.3643	593084.0558
S89°54'18"E 681.76'				
744	89+76.33	70' LT.	395638.2353	593765.8084
744	33+50.72	70' LT.	395638.2353	593765.8084
CURVE: R= 1979.859' Δ= 3°41'28" CB= N11°38'48"E C= 127.52' A= 127.55'				
761	34+73.76	70' LT.	395763.1345	593791.5529
N13°29'32.5"E 137.42'				
763	36+11.18	70' LT.	395896.7651	593823.6159
N01°41'10"E 200.66'				
764	38+07.59	111.06' LT.	396097.3362	593829.5204
S88°57'40"E 63.04'				
765	38+21.19	49.5' LT.	396096.1932	593892.5481
S88°57'40"E 50.69'				
773	38+32.12	ON CL	396095.2741	593943.2325
S88°57'40"E 54.50'				
774	38+43.87	53.22' RT.	396094.2860	593997.7210
N89°50'58"E 58.43'				
766	38+57.66	110' RT.	396094.4395	594056.1543
S13°29'32"W 383.90'				
762	34+73.76	110' RT.	395721.1377	593966.5851
CURVE: R= 1799.859' Δ= 11°48'22" CB= S07°35'21"W C= 370.22' A= 370.87'				
760	30+80.22	110' RT.	395354.1645	593917.6902
S01°41'10"W 130.22'				
758	29+50	110' RT.	395224.0008	593913.8584
S40°34'41"W 96.36'				
757	28+75	49.5' RT.	395150.8136	593851.1777
S77°30'50"W 51.055'				
772	28+62.50	ON CL	395139.7756	593801.3313
S77°30'50"W 51.055'				
756	28+50	49.5' LT.	395128.7375	593751.4849
N01°41'10"E 230.22'				
759	30+80.22	49.5' LT.	395358.8579	593758.2593
CURVE: R= 1959.359' Δ= 4°31'34" CB= N03°56'57"E C= 154.74' A= 154.78'				
745	32+31.09	49.5' LT.	395513.2300	593768.9167
745	89+79.64	55' RT.	395513.2300	593768.9167
N89°54'18"W 685.07'				
743	82+94.57	55' RT.	395514.3645	593083.8488
CURVE: R= 1692.022' Δ= 26°41'03" CB= N76°33'46.5"W C= 780.92' A= 788.03'				
741	75+32.16	55' RT.	395695.8336	592324.3022

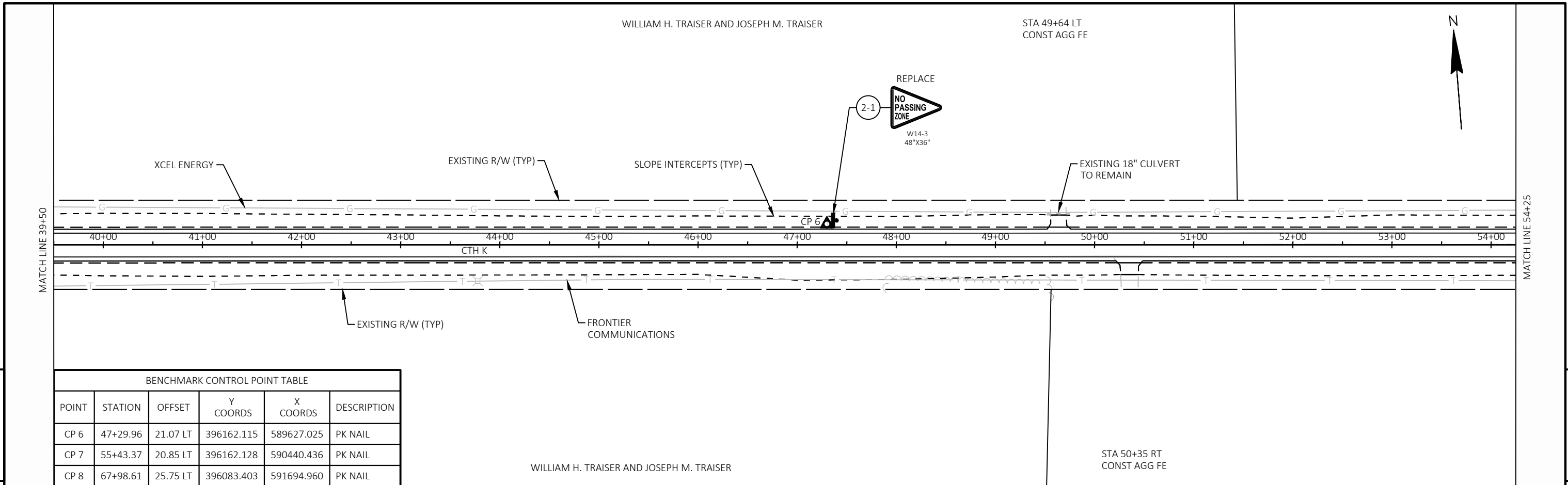
POINT NO.	STATION	OFFSET	Y COORDINATE	X COORDINATE
741	75+32.16	55' RT.	395695.8336	592324.3022
N63°13'15"W 192.88'				
739	73+39.28	55' RT.	395782.7360	592152.1099
N63°13'15"W 73.21'				
738	72+66.07	55' RT.	395815.7218	592086.7505
N63°13'15"W 224.36'				
737	70+41.71	55' RT.	395916.8109	591886.4483
CURVE: R= 1582.022' Δ= 12°46'33" CB= N69°36'31.5"W C= 352.03' A= 352.76'				
732	66+76.68	55' RT.	396039.4691	591556.4776
CURVE: R= 1582.022' Δ= 2°26'30" CB= N77°13'03"W C= 67.41' A= 67.42'				
731	66+06.92	55' RT.	396054.3848	591490.7332
CURVE: R= 1582.022' Δ= 11°24'34" CB= N84°08'35"W C= 314.51' A= 315.03'				
729	62+80.94	55' RT.	396086.4791	591177.8673
N01°33'50"E 10.00'				
728	62+81.19	45' RT.	396096.4784	591178.1403
CURVE: R= 1592.022' Δ= 0°10'39" CB= N89°55'39.5"W C= 4.93' A= 4.93'				
727	62+76.12	45' RT.	396096.4846	591173.2060
S89°59'01"W 2617.99'				
724	36+58.13	45' RT.	396095.7370	588555.2199
N79°41'07"W 66.91'				
723	35+92.30	33' RT.	396107.7182	588489.3876
S89°59'01"W 857.84'				
722	27+34.34	33' RT.	396107.4733	587631.5446
S89°46'34"W 728.64'				
720	20+05.82	33' RT.	396104.6252	586902.9118
S89°58'46"W 946.21'				
716	10+59.87	33' RT.	396104.2855	585956.6989
N89°33'29"W 60.10'				
714	9+99.77	33' RT.	396104.7491	585896.5999
S01°02'02"W 22.00'				
713	9+99.54	55' RT.	396082.7515	585896.2029
N89°33'29"W 756.16'				
712	2+43.38	55' RT.	396088.5845	585140.0614
N00°08'51"E 55.00'				
696	2+43.10	ON CL	396143.5850	585140.2031



BENCHMARK CONTROL POINT TABLE

POINT	STATION	OFFSET	Y COORDS	X COORDS	DESCRIPTION
CP 2	12+67.73	35.46 LT	396172.821	586164.803	PK NAIL
CP 3	19+29.89	21.98 LT	396159.575	586826.959	PK NAIL
CP 4	27+11.20	21.64 LT	396162.027	587608.187	PK NAIL
CP 5	36+26.41	21.76 LT	396162.490	588523.475	PK NAIL



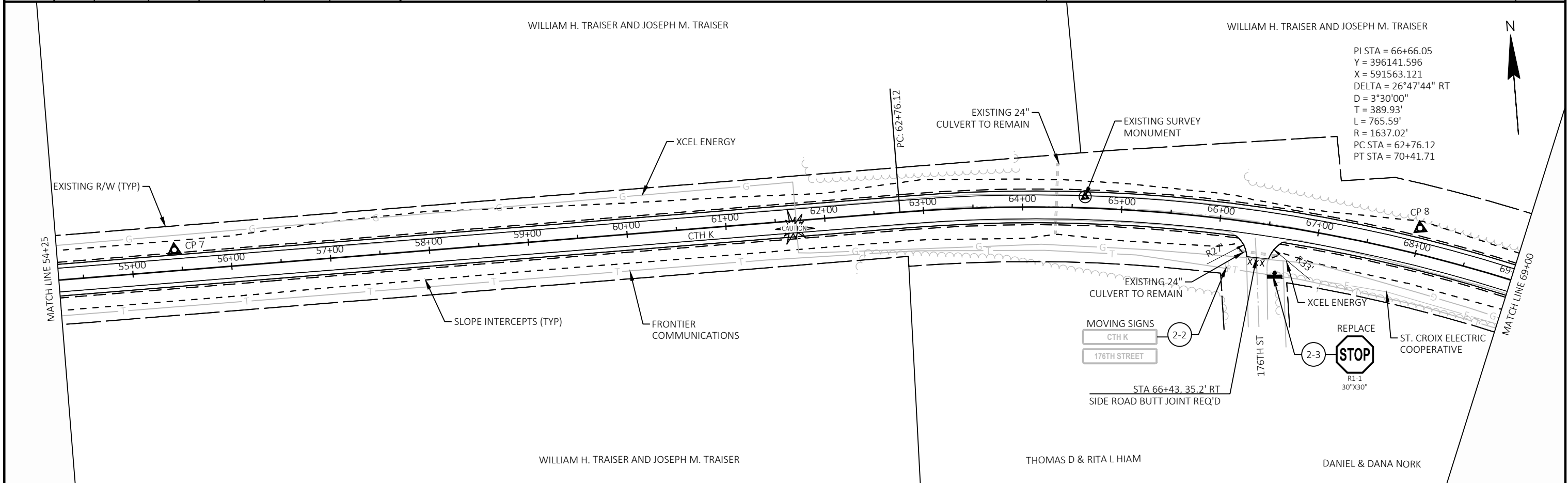


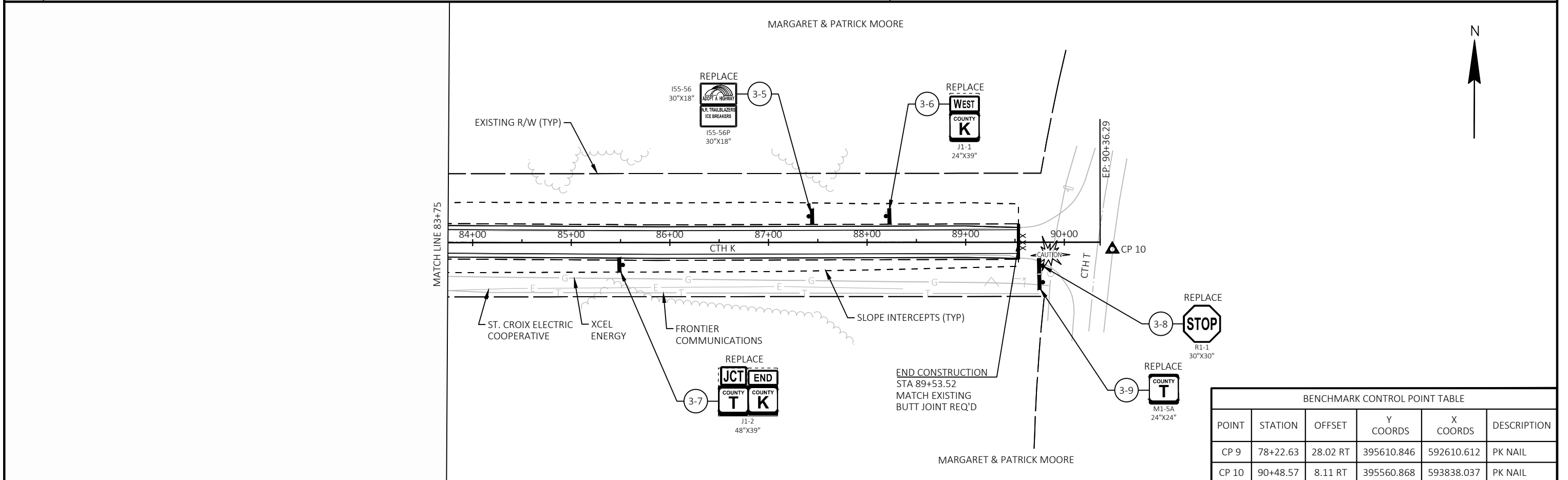
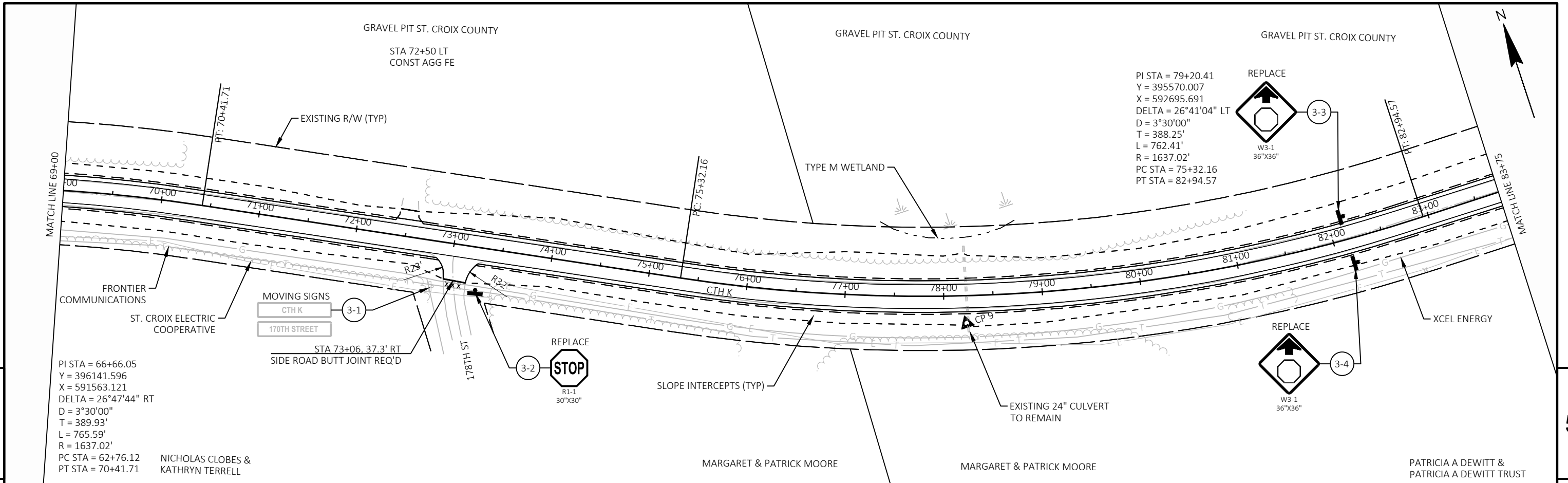
BENCHMARK CONTROL POINT TABLE

POINT	STATION	OFFSET	Y COORDS	X COORDS	DESCRIPTION
CP 6	47+29.96	21.07 LT	396162.115	589627.025	PK NAIL
CP 7	55+43.37	20.85 LT	396162.128	590440.436	PK NAIL
CP 8	67+98.61	25.75 LT	396083.403	591694.960	PK NAIL

5

5



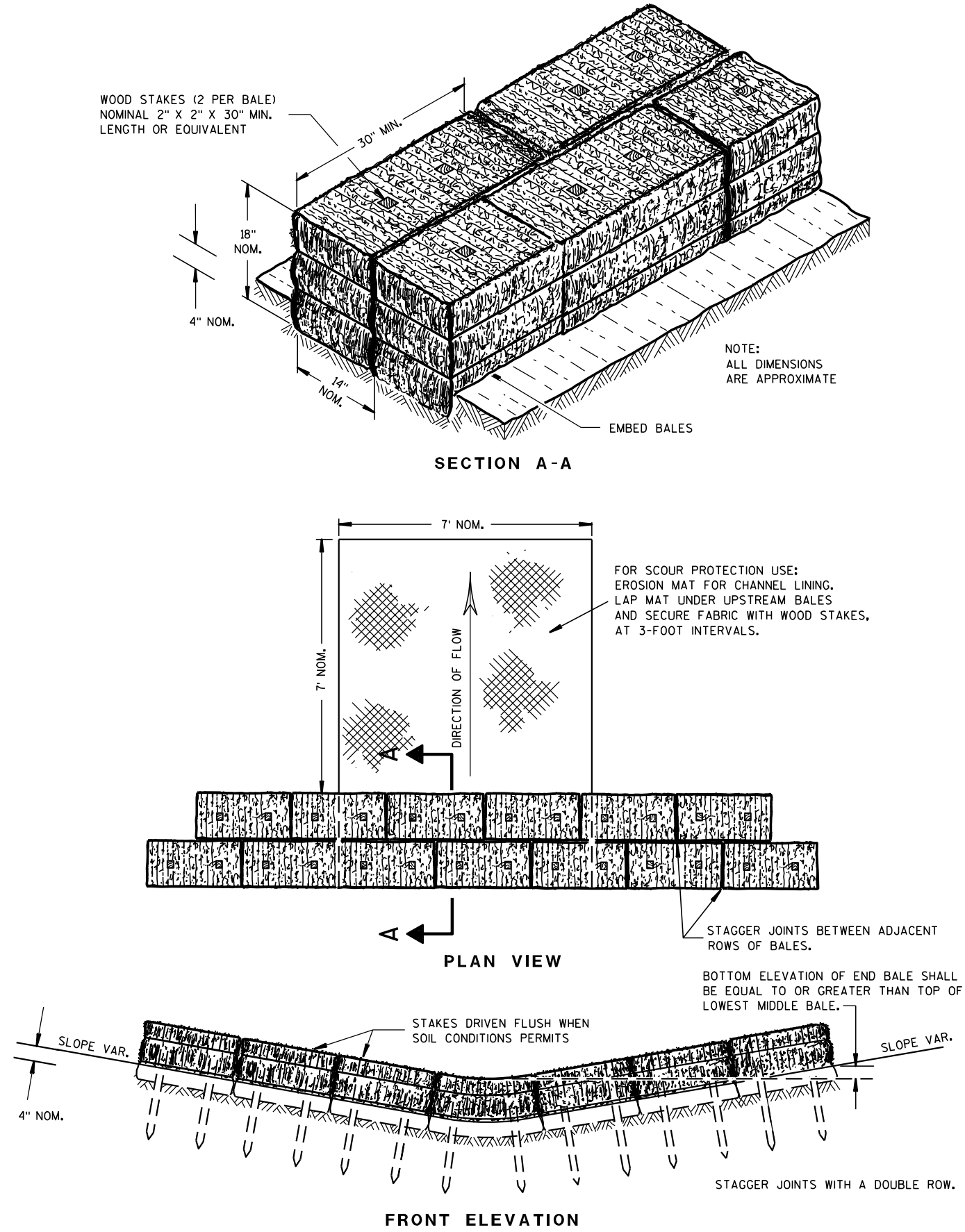


BENCHMARK CONTROL POINT TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	DESCRIPTION
CP 9	78+22.63	28.02 RT	395610.846	592610.612	PK NAIL
CP 10	90+48.57	8.11 RT	395560.868	593838.037	PK NAIL

PROJECT NO: 8941-05-71 HWY: CTH K COUNTY: ST CROIX PLAN SHEETS SHEET **E**

Standard Detail Drawing List

08E08-03	TYPICAL INSTALLATIONS OF EROSION BALES / TEMPORARY DITCH CHECKS
08E09-06	SILT FENCE
08E15-01	CULVERT PIPE CHECK
09A01-14A	AT-GRADE SIDE ROAD INTERSECTION, TYPES "B1", "B2", "C" AND D AND TEE INTERSECTION BYPASS LANE
13C19-03	HMA LONGITUDINAL JOINTS
14B42-07A	MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL
14B42-07B	MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL
14B42-07C	MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL
14B42-07D	MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL
14B44-04A	MIDWEST GUARDRAIL SYSTEM ENERGY ABSORBING TERMINAL (MGS)
14B44-04B	MIDWEST GUARDRAIL SYSTEM ENERGY ABSORBING TERMINAL (MGS)
14B44-04C	MIDWEST GUARDRAIL SYSTEM ENERGY ABSORBING TERMINAL (MGS)
14B45-05A	MIDWEST GUARDRAIL SYSTEM THREE BEAM TRANSITION (MGS)
14B45-05B	MIDWEST GUARDRAIL SYSTEM THREE BEAM TRANSITION (MGS)
14B45-05C	MIDWEST GUARDRAIL SYSTEM THREE BEAM TRANSITION (MGS)
14B45-05D	MIDWEST GUARDRAIL SYSTEM THREE BEAM TRANSITION (MGS)
14B45-05E	MIDWEST GUARDRAIL SYSTEM THREE BEAM TRANSITION (MGS)
14B45-05F	MIDWEST GUARDRAIL SYSTEM THREE BEAM TRANSITION (MGS)
14B45-05G	MIDWEST GUARDRAIL SYSTEM THREE BEAM TRANSITION (MGS)
14B45-05H	MIDWEST GUARDRAIL SYSTEM THREE BEAM TRANSITION (MGS)
14B45-05I	MIDWEST GUARDRAIL SYSTEM THREE BEAM TRANSITION (MGS)
14B45-05J	MIDWEST GUARDRAIL SYSTEM THREE BEAM TRANSITION (MGS)
14B45-05K	MIDWEST GUARDRAIL SYSTEM THREE BEAM TRANSITION (MGS)
14B45-05L	MIDWEST GUARDRAIL SYSTEM THREE BEAM TRANSITION (MGS)
15A03-02A	FLEXIBLE MARKER POST FOR CULVERT END
15A03-02B	FLEXIBLE MARKER POST FOR CULVERT END
15C02-09A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-09B	BARRICADES AND SIGNS FOR VARIOUS CLOSURES
15C02-09C	DETOUR SIGNING FOR MAINLINE CLOSURES
15C03-05	BARRICADES AND SIGNS FOR SIDEROAD CLOSURES
15C08-23A	PERMANENT LONGITUDINAL PAVEMENT MARKINGS
15C19-08A	MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY
15C35-06A	PAVEMENT MARKING (INTERSECTIONS)

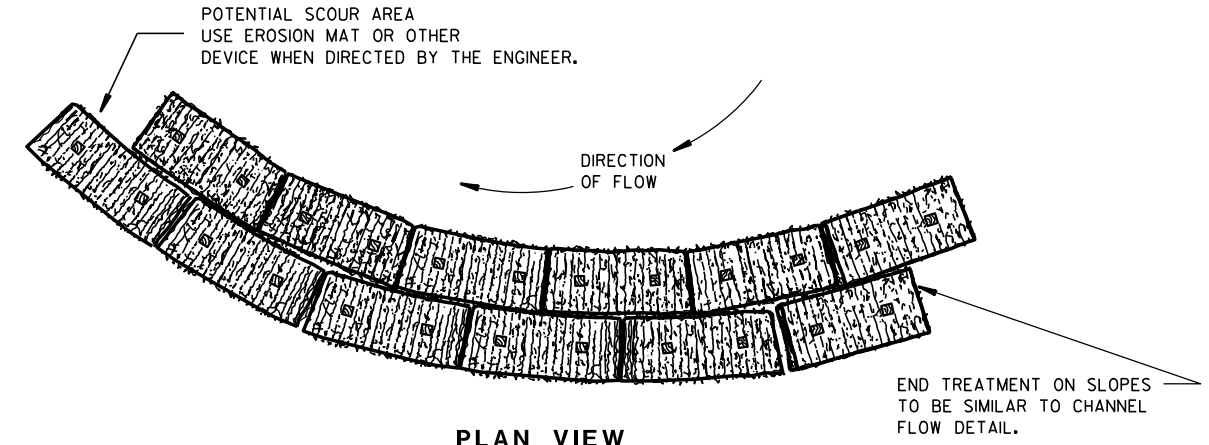


TEMPORARY DITCH CHECK USING EROSION BALES ①

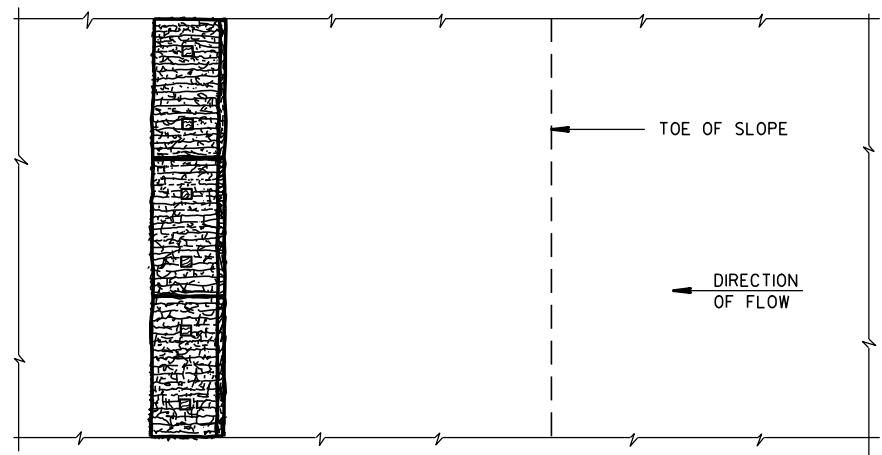
GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

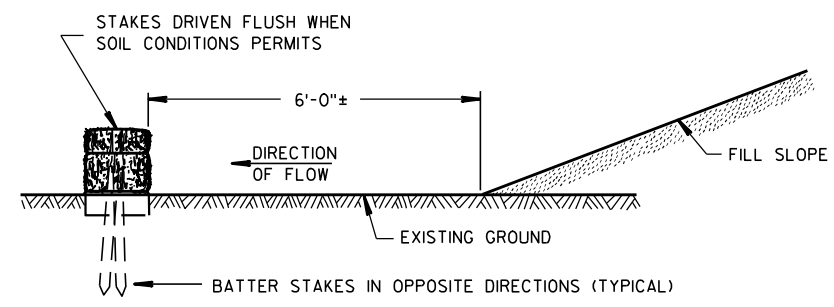
- ① TEMPORARY DITCH CHECKS EITHER EROSION BALES OR MANUFACTURED SHALL BE PAID FOR UNDER THE BID ITEM OF TEMPORARY DITCH CHECK. THE DEPARTMENT WILL NOT PAY FOR TEMPORARY DITCH CHECKS CONSTRUCTED OF A SINGLE ROW OF EROSION BALES.



PLAN VIEW WHEN ALTERING THE DIRECTION OF FLOW



PLAN VIEW

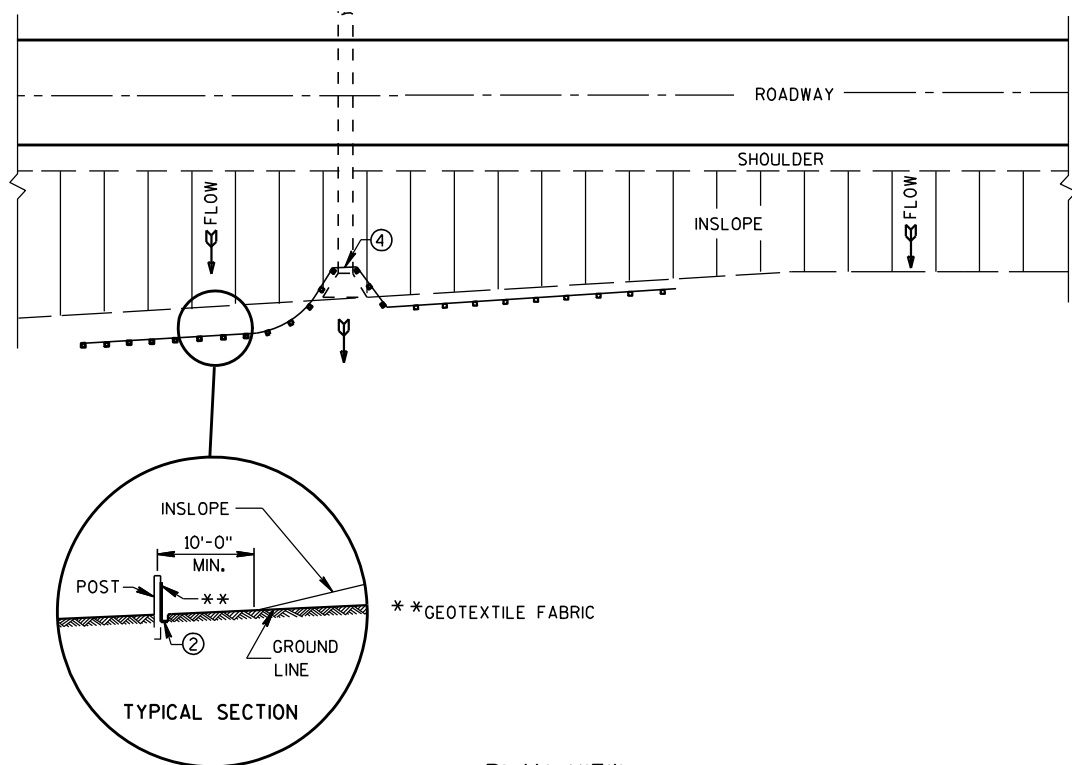


FRONT ELEVATION WHEN EXISTING GROUND SLOPES AWAY FROM FILL SLOPE EROSION BALES FOR SHEET FLOW

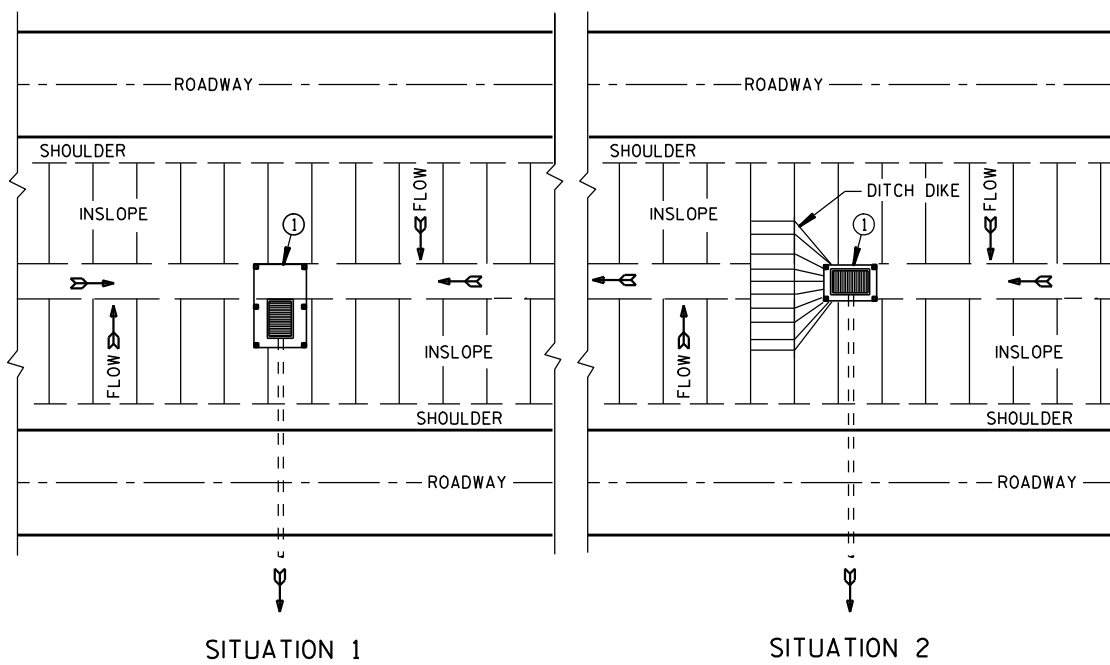
TYPICAL INSTALLATIONS OF EROSION BALES / TEMPORARY DITCH CHECKS

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED
 6/04/02 /S/ Beth Canestra
 DATE CHIEF ROADWAY DEVELOPMENT ENGINEER
 FHWA



PLAN VIEW
TYPICAL APPLICATION OF SILT FENCE

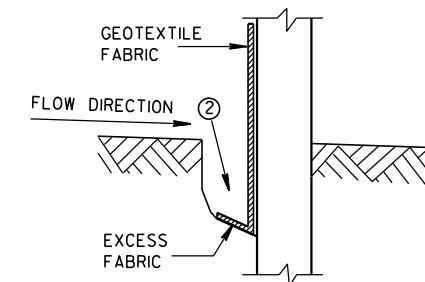


SITUATION 1 SITUATION 2
PLAN VIEW
SILT FENCE AT MEDIAN SURFACE DRAINS

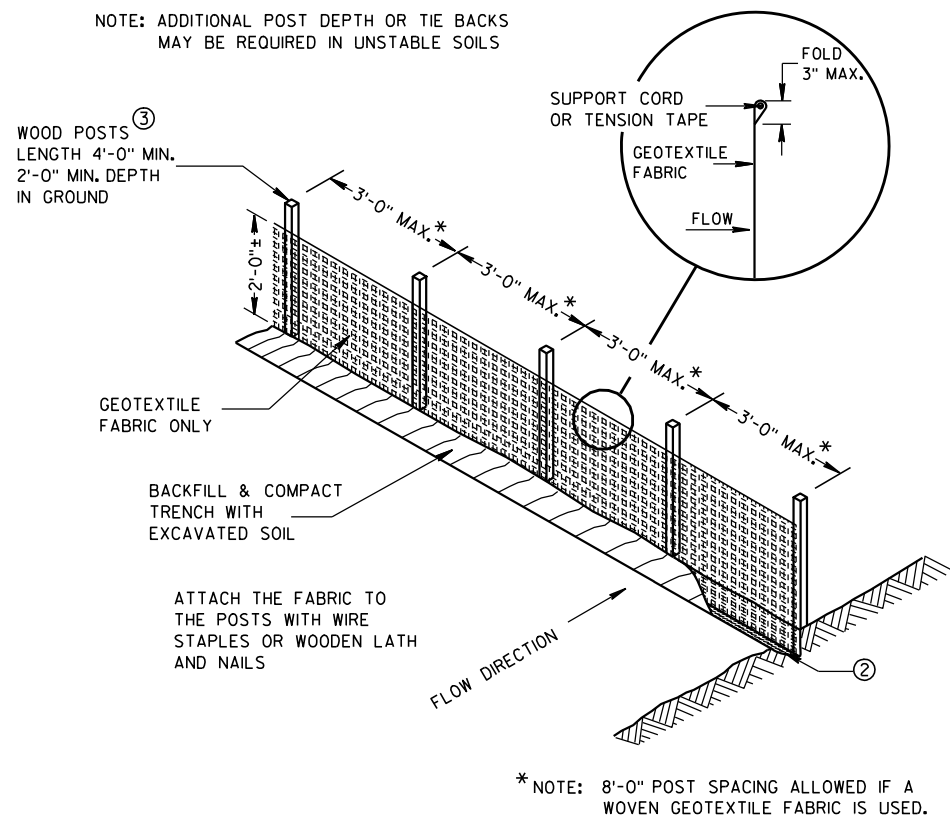
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

- ① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- ③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1 1/8" X 1 1/8" OF OAK OR HICKORY.
- ④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- ⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.

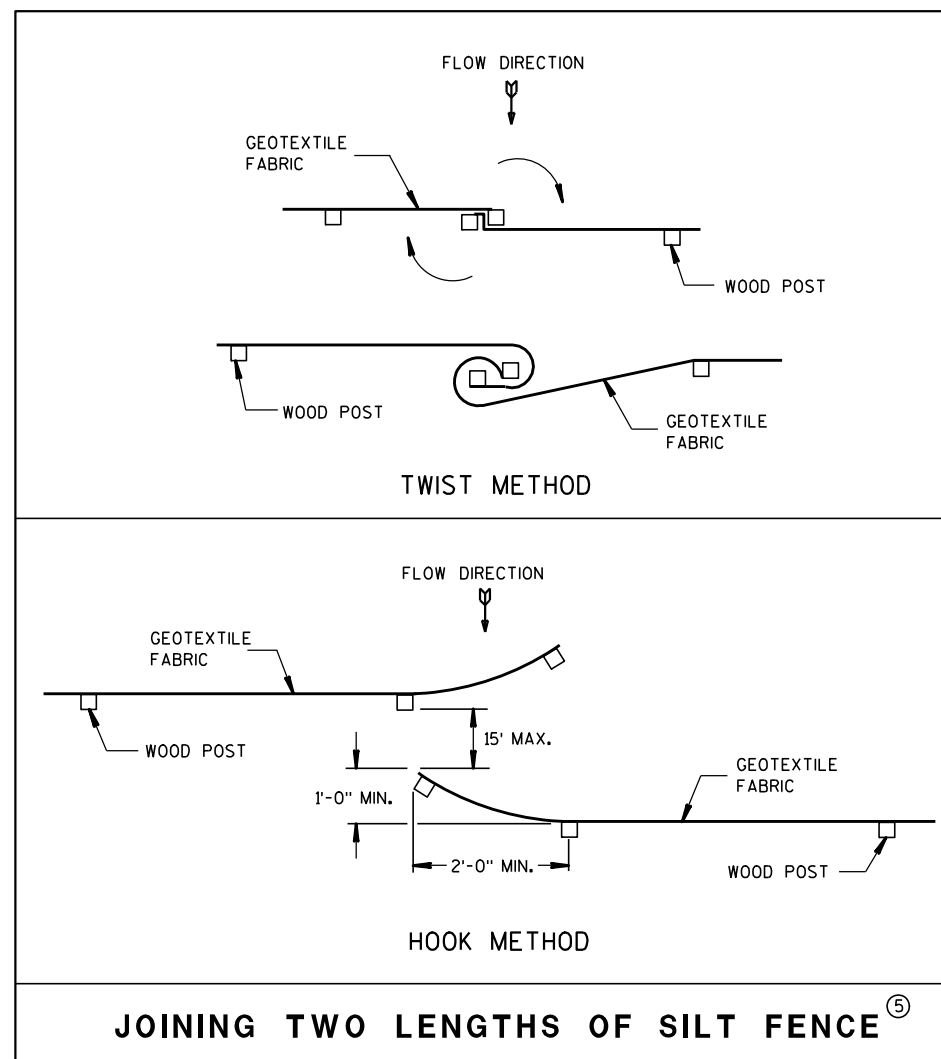


TRENCH DETAIL

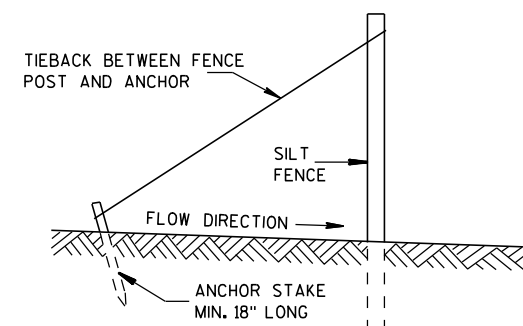


SILT FENCE

* NOTE: 8'-0" POST SPACING ALLOWED IF A WOVEN GEOTEXTILE FABRIC IS USED.



JOINING TWO LENGTHS OF SILT FENCE ⑤

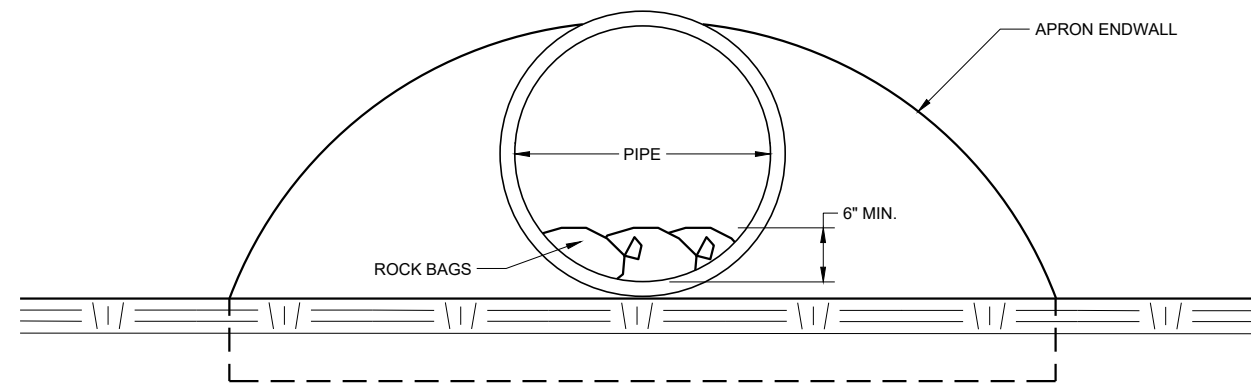


SILT FENCE TIE BACK
(WHEN REQUIRED BY THE ENGINEER)

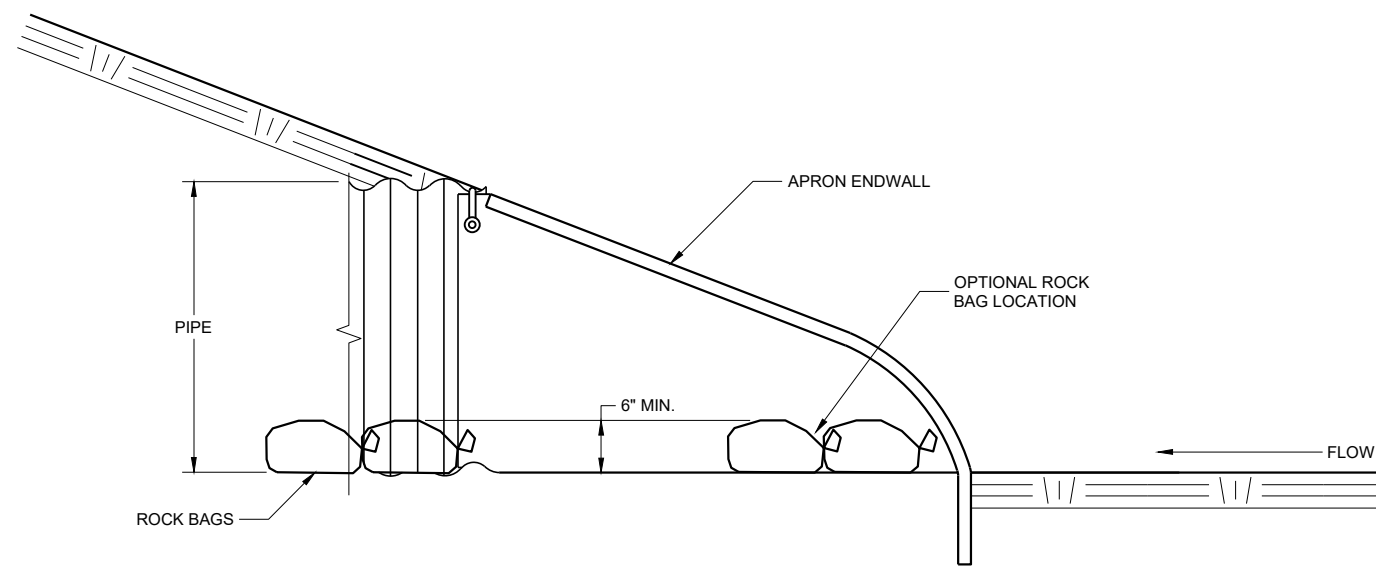
SILT FENCE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
4-29-05 /S/ Beth Canestra
DATE CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA



END VIEW



SIDE VIEW

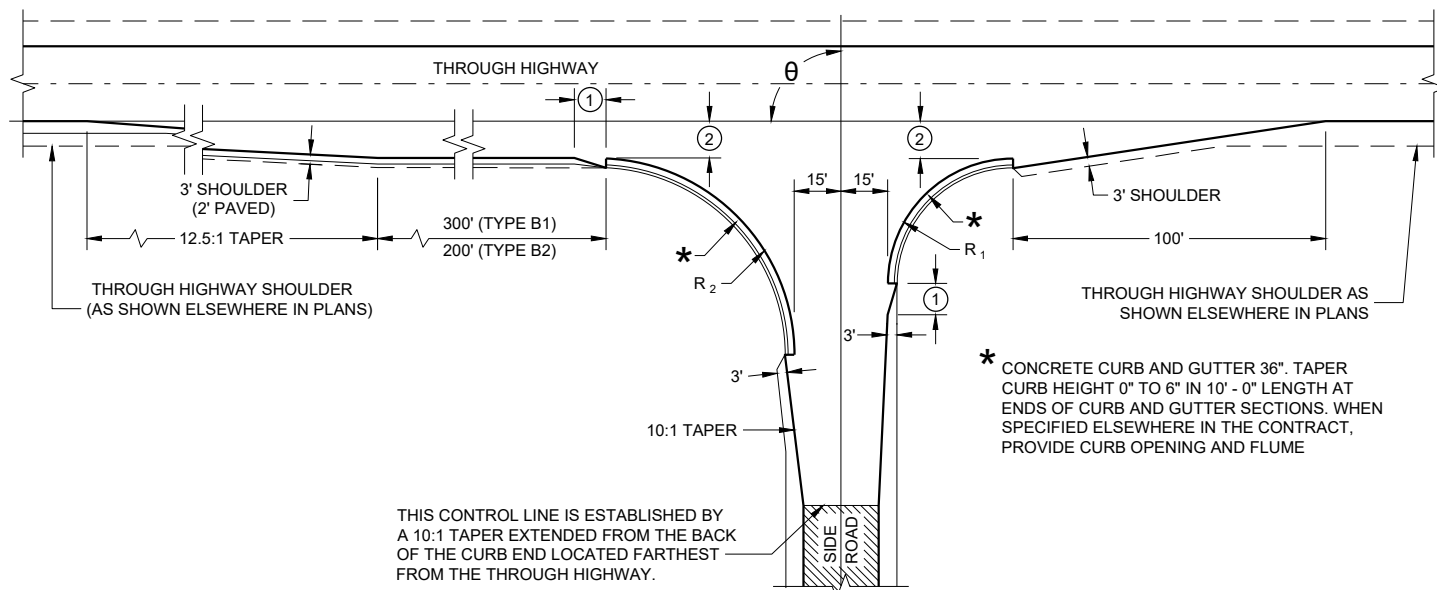
CULVERT PIPE CHECK
(INSTALL ON INLET END ONLY)

CULVERT PIPE CHECK

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2019 /S/ Daniel Schave
DATE EROSION CONTROL ENGINEER

FHWA



TYPE "B1" AND "B2"

RADI DIMENSIONS FOR TYPES "B1", "B2", "C" AND "D" INTERSECTIONS

θ	R_1	R_2
65 - 70	35	70
71 - 80	40	70
81 - 90	40	60
91 - 100	50	55
101 - 110	60	45

GENERAL NOTES

DESIGNS MAY BE USED INTERCHANGEABLY IN COMBINATION OR SEPARATELY FOR ANY ONE COMPLETE INTERSECTION DEPENDING UPON INTERSECTION ANGLE AND SURFACING OF EACH APPROACH ROADWAY.

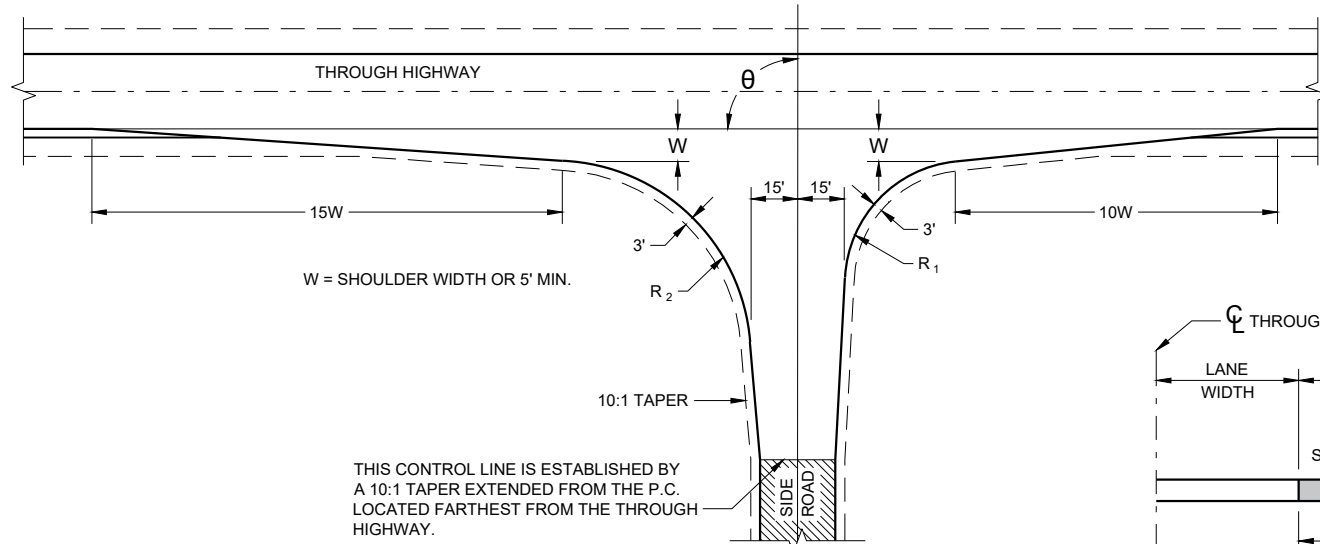
SIDE ROAD SURFACING NOTE

WHEN THE SIDE ROAD IS NOT PRESENTLY PAVED, PAVEMENT SHALL BE PLACED TO THE LIMITS SHOWN UNLESS OTHERWISE PROVIDED IN THE CONTRACT. WHERE THE CONSTRUCTION LIMITS ARE BEYOND THE PAVING LIMITS, CRUSHED AGGREGATE SURFACING SHALL BE PLACED BETWEEN THE PAVING LIMITS AND CONSTRUCTION LIMITS.

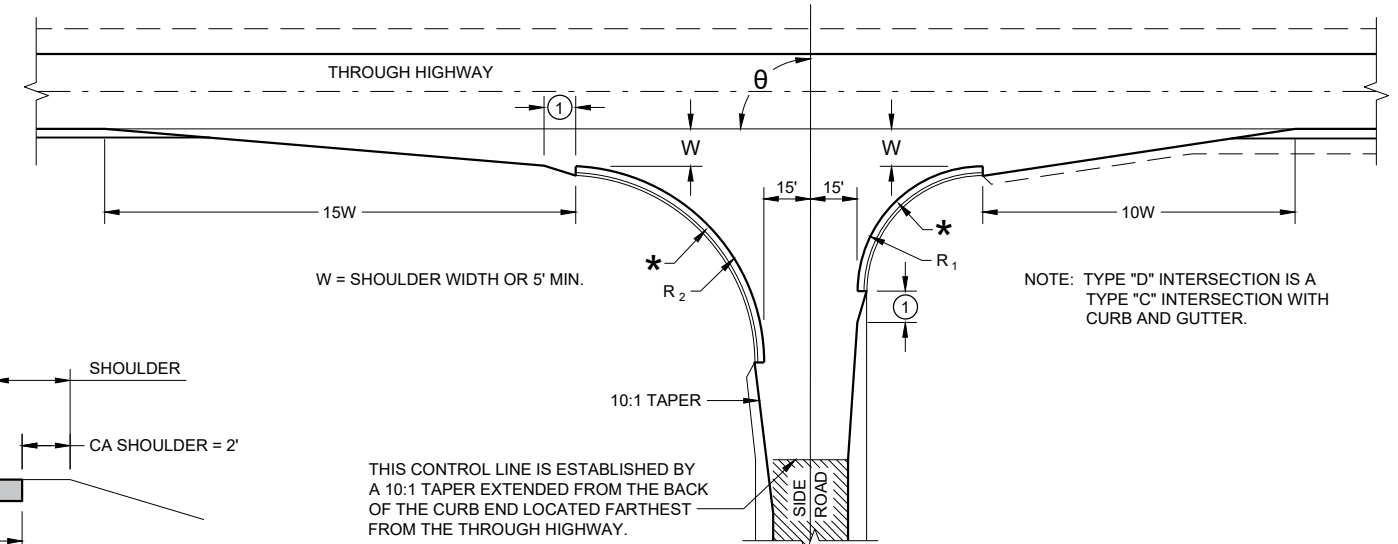
WHEN THE SIDE ROAD IS PRESENTLY PAVED, NEW PAVEMENT SHALL BE PLACED TO THE LIMITS OF DESIGN AS SHOWN AND BEYOND, IF NECESSARY, TO MEET EXISTING PAVEMENT.

WHEN THE SIDE ROAD IS THE CONSTRUCTION PROJECT, THE INTERSECTION SURFACING SHALL BE THE SAME AS FOR THE PROJECT.

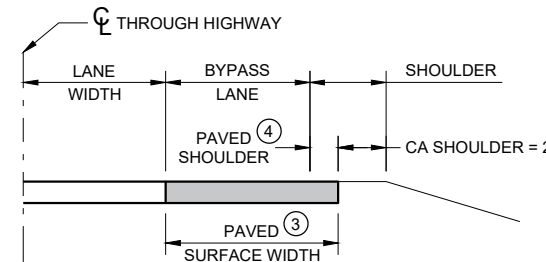
- ① 10-FT TYPICAL.
- ② 12-FT** PLUS ADDITIONAL WIDTH FOR BIKE LANE IF SHOWN ELSEWHERE IN THE PLAN.
**10-FT MAY BE USED ON TYPE B2 ON RESURFACING PROJECTS IF SPECIFIED IN THE CONTRACT.
- ③ BYPASS LANE PAVED SURFACE WIDTH OUTSIDE OF TRAVEL LANE
- ASPHALT = 12-FT PLUS PAVED SHOULDER WIDTH
- PC CONCRETE = 13-FT PLUS PAVED SHOULDER WIDTH
- ④ BYPASS LANE PAVED SHOULDER WIDTH = THE GREATER OF 1-FT OR THE PAVED SHOULDER WIDTH OF THE THROUGH HIGHWAY.



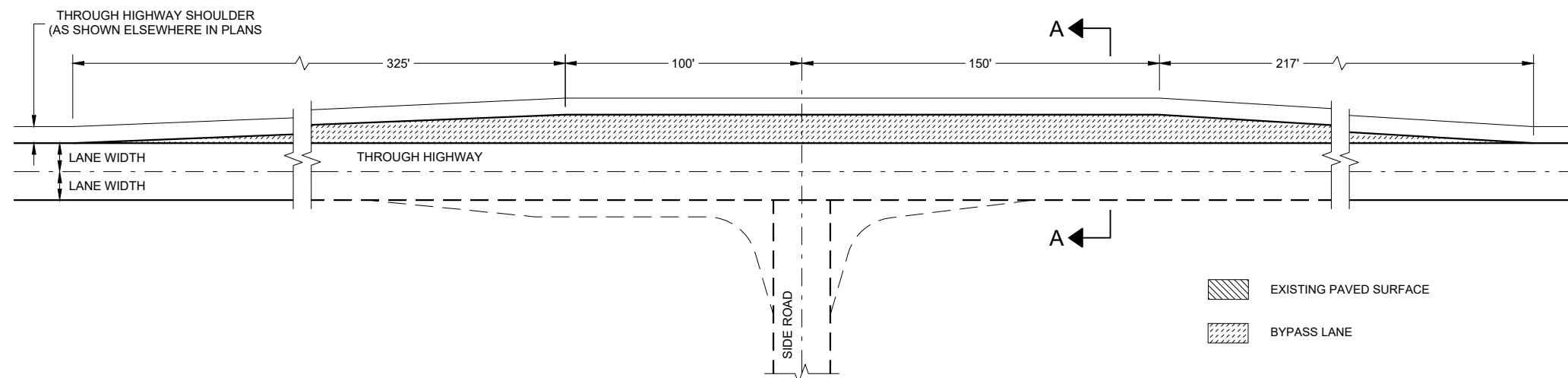
TYPE "C"



TYPE "D"



SECTION A - A
(SHOWING BYPASS LANE AND SHOULDER)

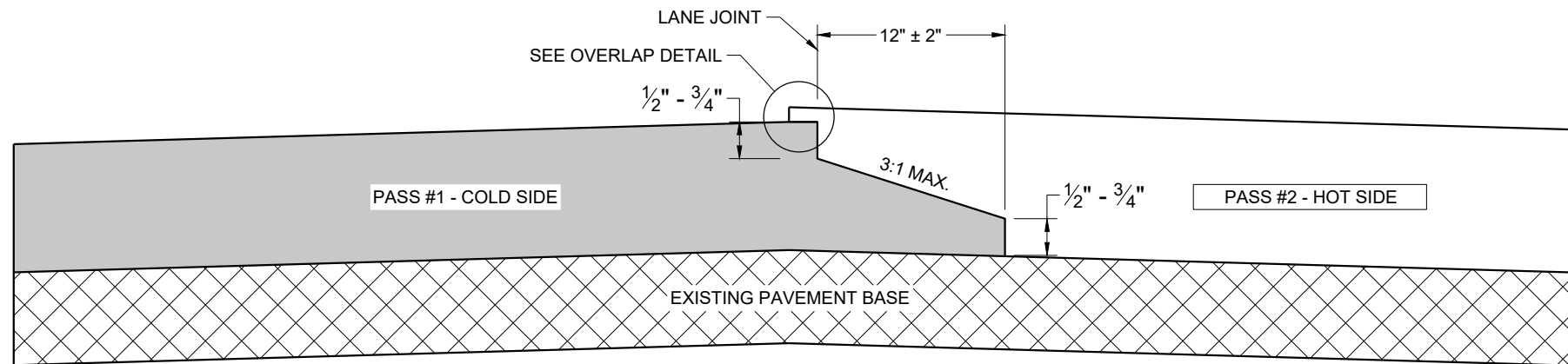


TEE INTERSECTION BYPASS LANE DETAIL

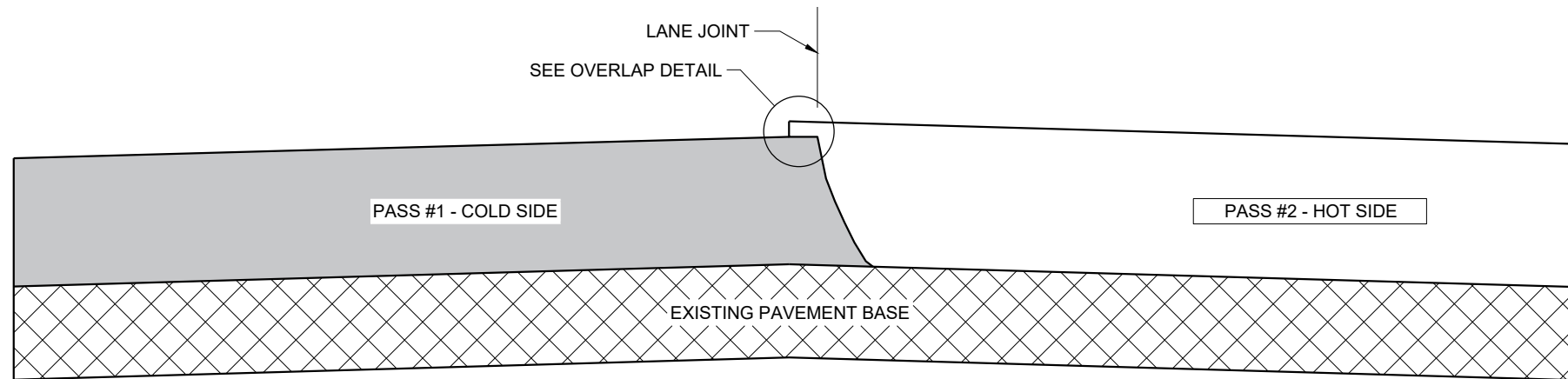
- EXISTING PAVED SURFACE
- BYPASS LANE

AT GRADE SIDE ROAD INTERSECTION TYPES "B1", "B2", "C", "D" AND TEE INTERSECTION BYPASS LANE

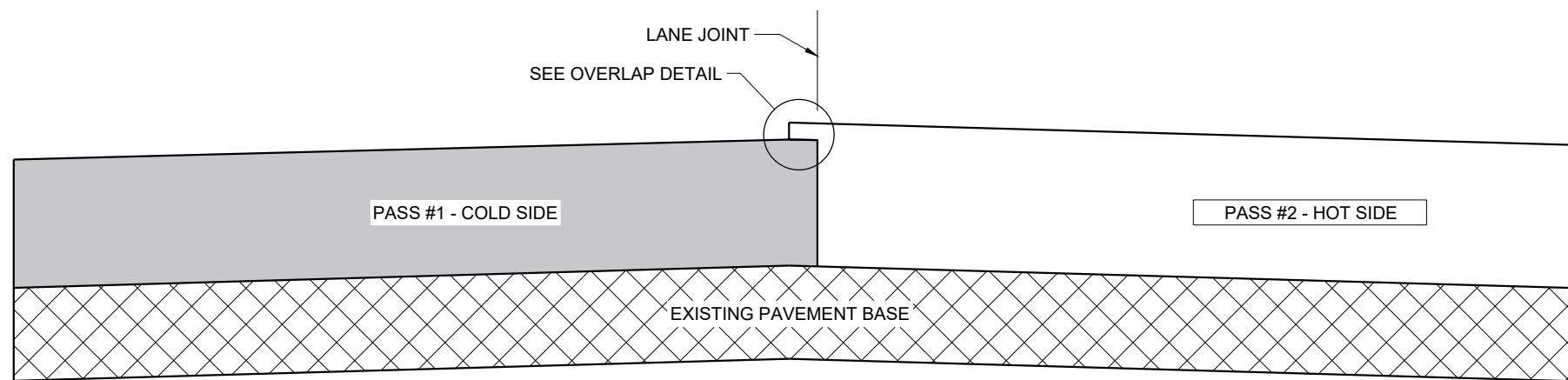
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



**TYPICAL PAVEMENT CROSS SECTION
NOTCHED WEDGE JOINT**



**TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT**



**TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT (MILLED)**

GENERAL NOTES

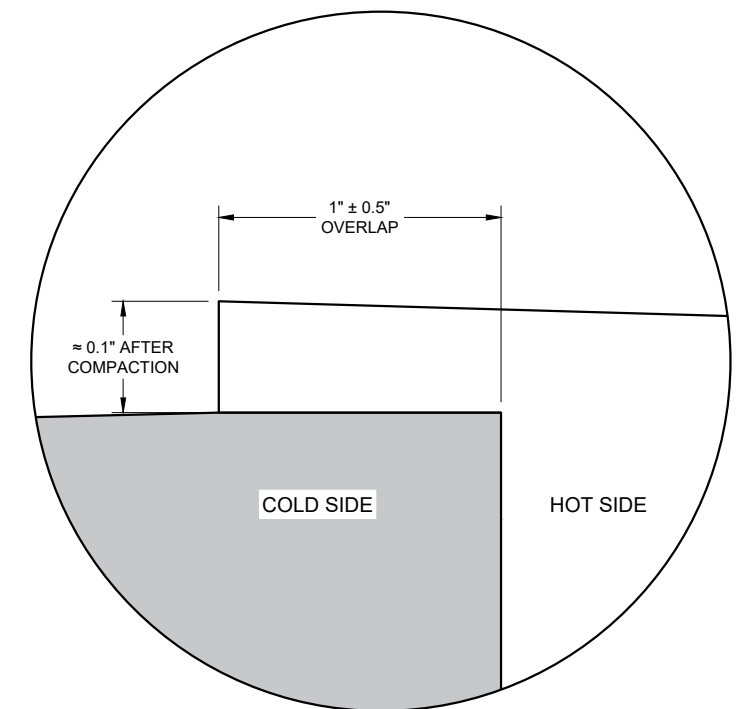
IN ADDITION TO THE DETAILS PROVIDED IN THIS DRAWING, CONFORM TO STANDARD SPECIFICATION 450.3.2.8 FOR WHEN A NOTCHED WEDGE JOINT IS REQUIRED AND FOR GENERAL JOINT CONSTRUCTION REQUIREMENTS.

FOR ALL LONGITUDINAL JOINTS, ENSURE THE PAVER SCREED OVERLAPS THE PREVIOUSLY PLACED PAVEMENT BY $1" \pm 0.5"$ AND THE HOT SIDE OF THE JOINT REMAINS HIGHER THAN THE COLD SIDE BY APPROXIMATELY 0.1" AFTER FINAL COMPACTION. (IT WILL BE FLUSH WHEN PAVING IN ECHELON.)

ONLY REMOVE THE LONGITUDINAL NOTCHED WEDGE JOINT FOR SMA PAVEMENT OR AS DIRECTED BY THE ENGINEER TO ADDRESS SPECIFIC LENGTHS OF JOINT DAMAGED BY TRAFFIC.

WHEN MILLING BACK OR REMOVING ANY LONGITUDINAL JOINT, LIMIT THE MATERIAL REMOVED TO 2" FROM THE TOP NOTCH OR FROM THE VERTICAL JOINT EDGE ON THE COLD SIDE OF THE JOINT.

USE LONGITUDINAL MILLED JOINT AS PLANS SHOW OR THE AS THE ENGINEER DIRECTS.



OVERLAP DETAIL (TYPICAL)

6

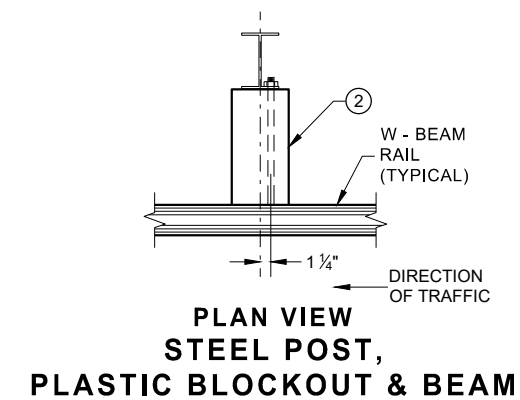
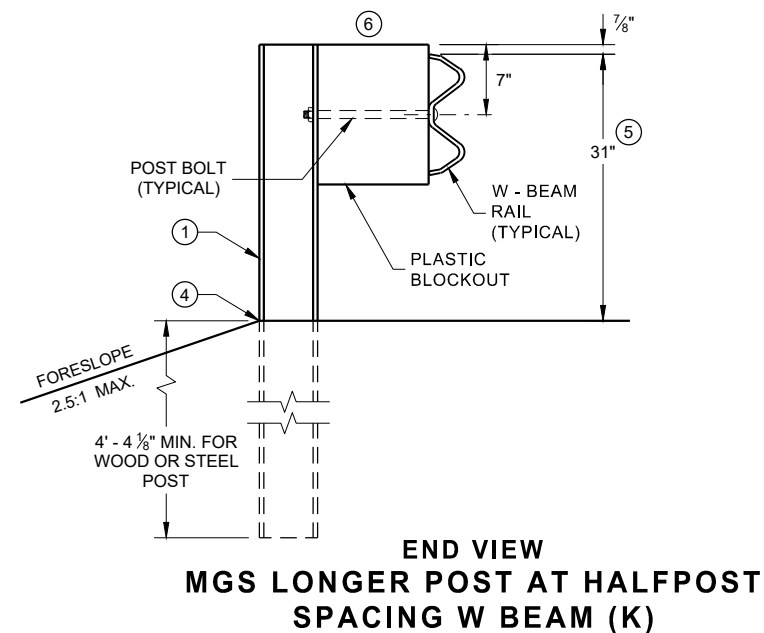
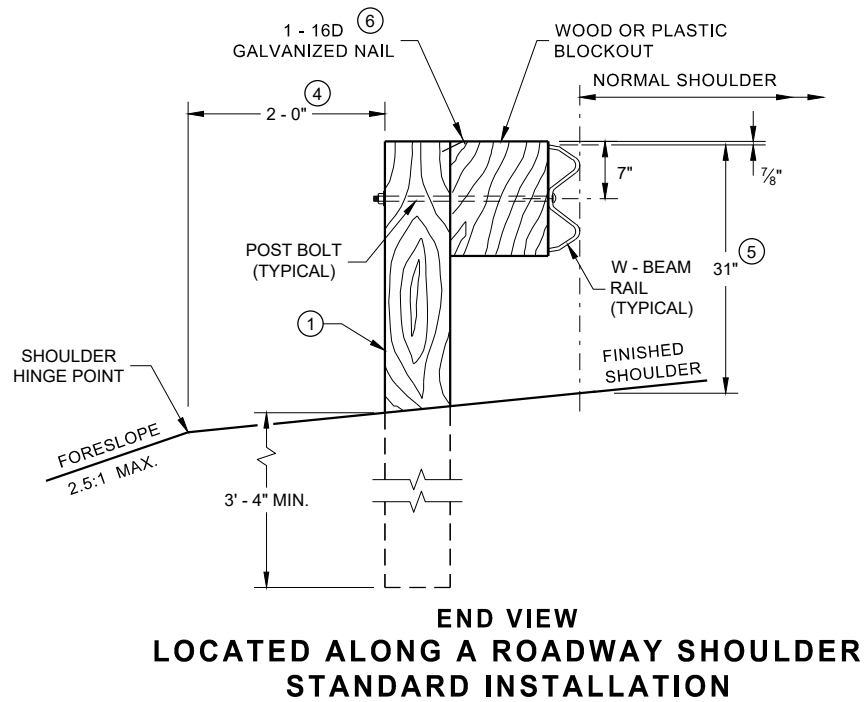
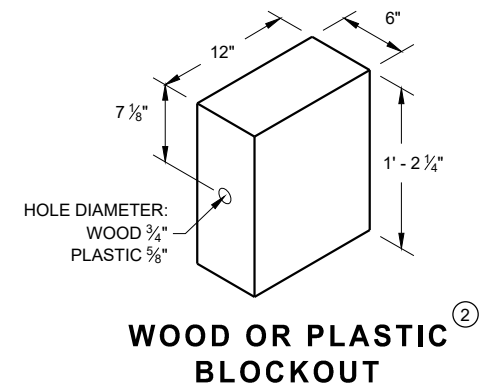
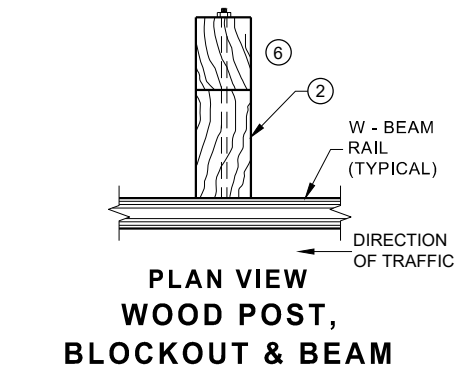
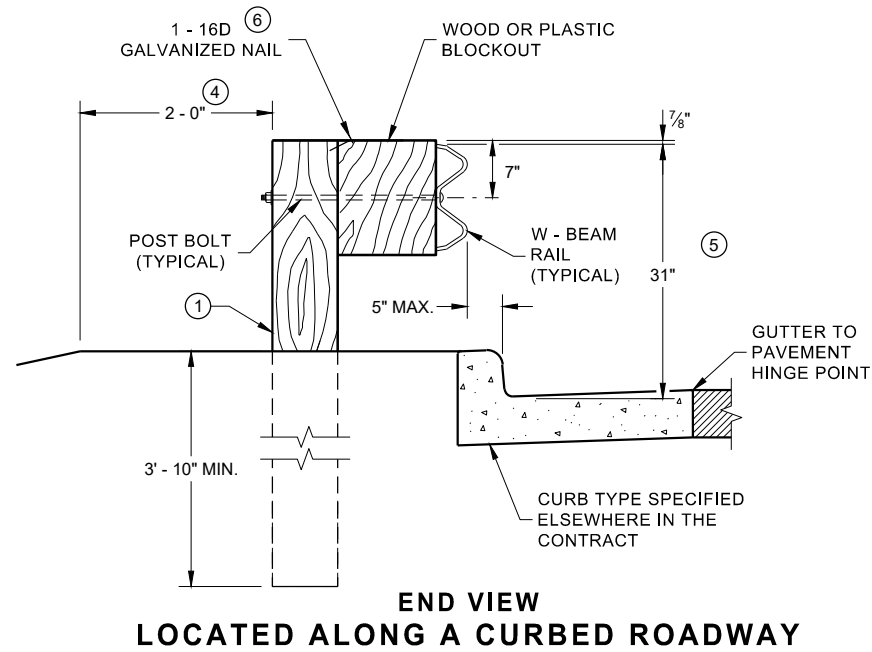
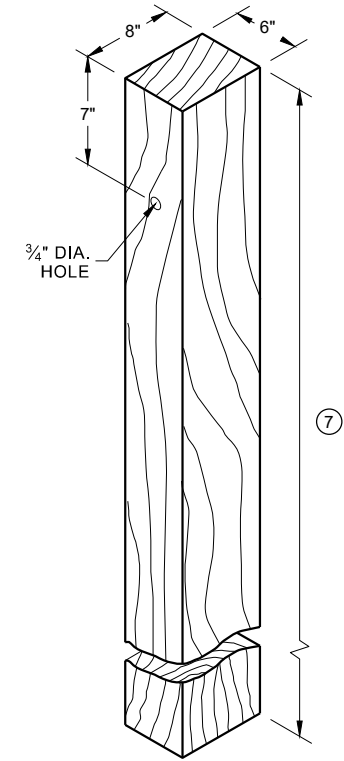
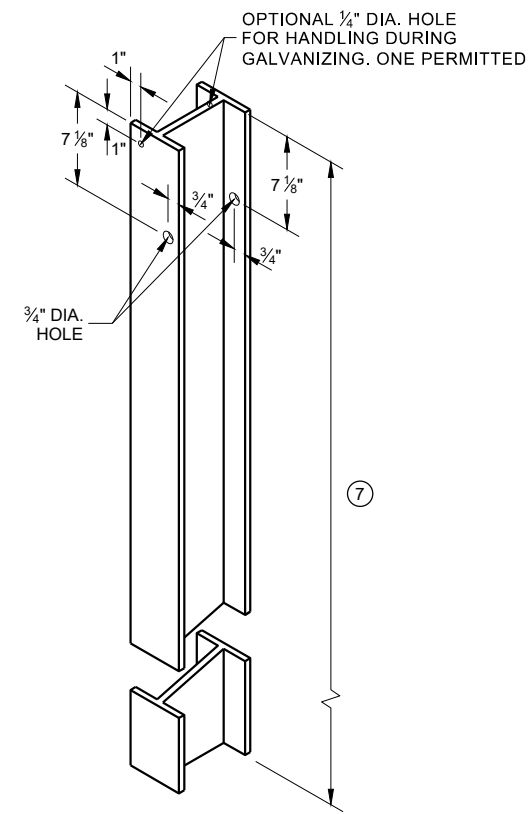
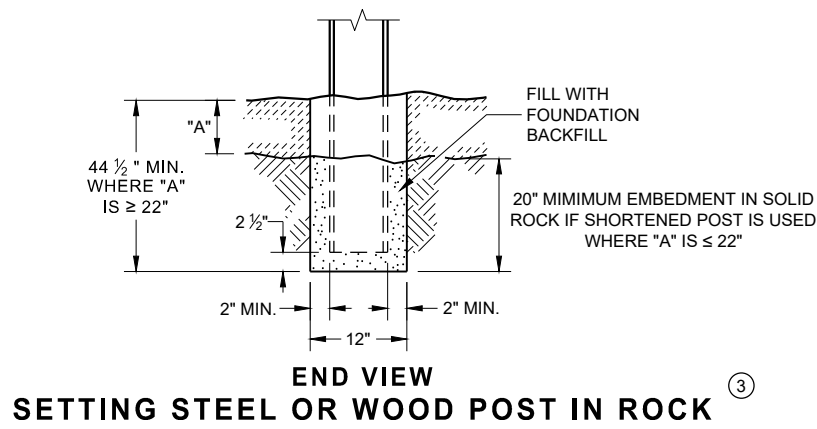
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SDD 13C19 - 03

SDD 13C19 - 03

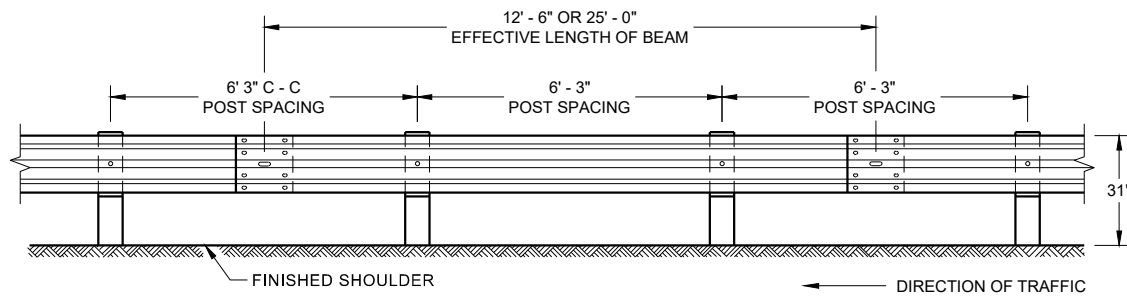
HMA LONGITUDINAL JOINTS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED November 2020 DATE	/S/ Steven Hefel HMA PAVEMENT ENGINEER
FHWA	

- ① WOOD OR STEEL POSTS (w6X9 OR w6X8.5) MAY BE USED. DO NOT INTERMIX WOOD AND STEEL POSTS. INSTALL STEEL POSTS WITH HOLES ON APPROACHING TRAFFIC SIDE.
- ② USE WOOD OR APPROVED PLASTIC BLOCKOUTS. WOOD BLOCKOUTS MAY BE CONSTRUCTED OUT OF TWO OR MORE WOOD BLOCKOUTS. SEE ALTERNATE WOOD BLOCKOUT DETAIL. DIMENSIONS OF APPROVED PLASTIC BLOCKOUTS MAY VARY.
- ③ IF ROCK IS ENCOUNTERED DURING EXCAVATION, PROVIDE A HOLE 12 INCHES IN DIAMETER EXTENDING 20 INCHES DEEP INTO THE ROCK. PLACE APPROXIMATELY 2 1/2" INCHES OF GRANULAR MATERIAL IN THE BOTTOM OF THE HOLE. CUT THE POSTS THE TO LENGTH AND INSTALL. BACKFILL WITH EXCAVATED MATERIAL AND COMPACT. BACKFILL IS TO BE FREE OF LARGE ROCKS.
- ④ WHEN THE DISTANCE FROM BACK OF POST TO SHOULDER HINGE POINT IS LESS THAN 2 FEET INSTALL LONGER POST AT HALF POST SPACING (K).
- ⑤ FOR NEW MGS INSTALLATION TOP OF W-BEAM RAIL TOLERANCE IS $\pm 1"$. FOR EXISTING MGS INSTALLATION TOP OF W-BEAM IS BETWEEN 27 3/4" TO 32".
- ⑥ WHEN USING STEEL POST AND WOOD BLOCKOUTS INSTALL FOUR 16D GALVANIZED NAILS. INSTALL NAILS AT THE BACK CORNERS OF THE BLOCK AND BEND THE NAILS OVER THE FLANGE OF THE STEEL POST.
- ⑦ TOTAL POST LENGTH FOR TYPE K IS 7' - 0". TOTAL POST LENGTH FOR OTHER MGS TYPES IS 6' - 0".

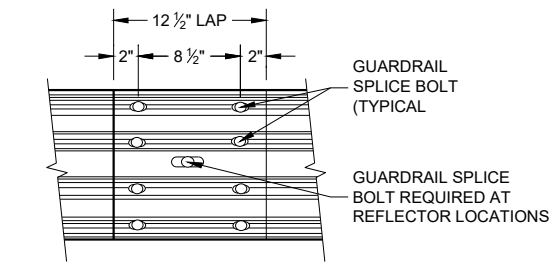


**MIDWEST GUARDRAIL SYSTEM
(MGS) GUARDRAIL**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



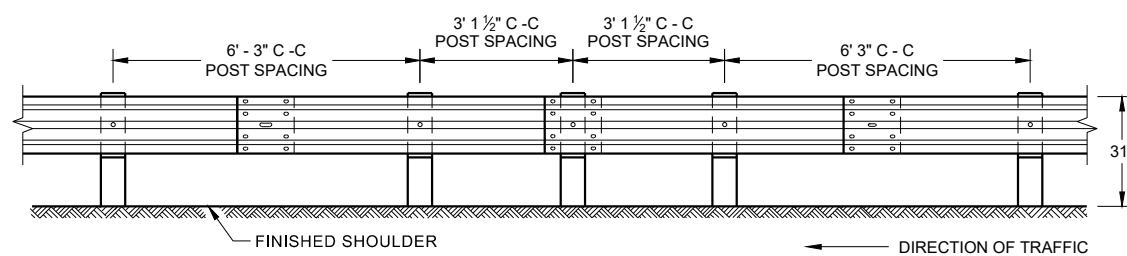
**FRONT VIEW
POST SPACING STANDARD INSTALLATION**



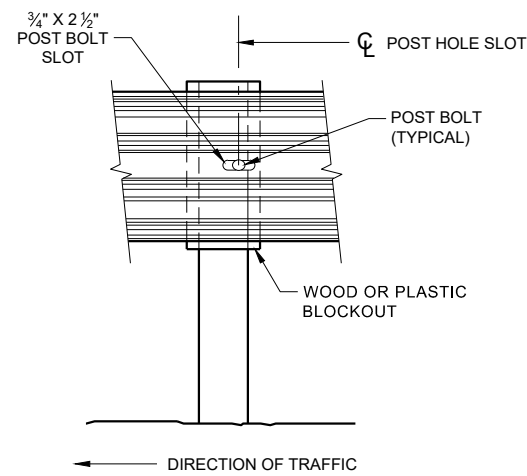
**FRONT VIEW
MID-SPAN BEAM SPLICE**

GENERAL NOTES

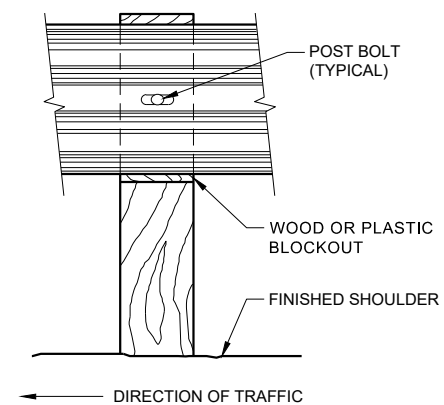
- ⑧ DO NOT INSTALL REFLECTORS ON THE FIRST 50 FEET OF THE APPROACH END OF THE ENERGY ABSORBING TERMINAL.
 - ⑨ 25 FEET OF HALF POST SPACING IS REQUIRED ON APPROACH AND DEPARTURE ENDS OF QUARTER POST SPACING.
- POST BOLTS ARE A 3/8" DIAMETER ASTM A307 GUARDRAIL BOLT. A POST BOLT REQUIRES 3/4" DIAMETER A563A DOUBLE RECESSED (DR) HEAVY HEX NUT AND 3/8" DIAMETER F844 FLAT WASHER. POST BOLTS MAY BE LONGER IF MULTIPLE BLOCKOUTS ARE BEING USED.
- GUARD RAIL SPLICE BOLTS ARE A 3/8" DIAMETER ASTM A307 GUARDRAIL HEAD BOLT. A GUARDRAIL SPLICE BOLT REQUIRES 3/8" DIAMETER A563A DOUBLE RECESSED (DR) HEAVY HEX NUT.



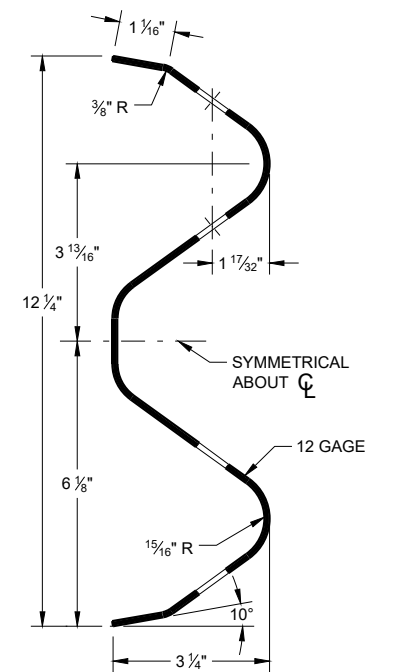
**FRONT VIEW
HALF POST SPACING (HS) AND
HALF POST SPACING WITH LONGER POSTS (K)**



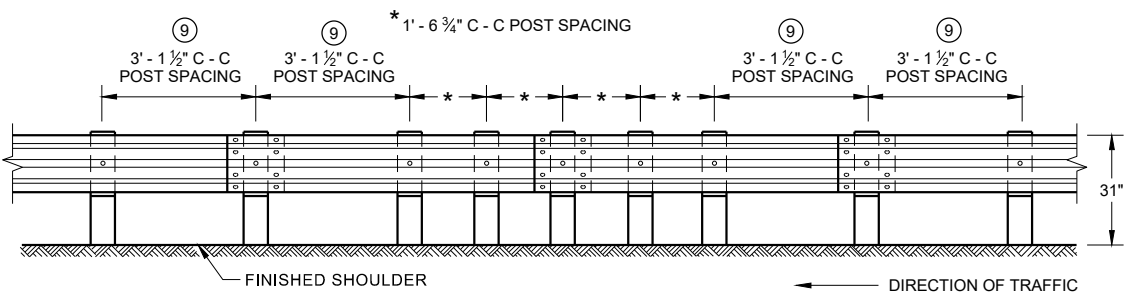
FRONT VIEW AT STEEL POST



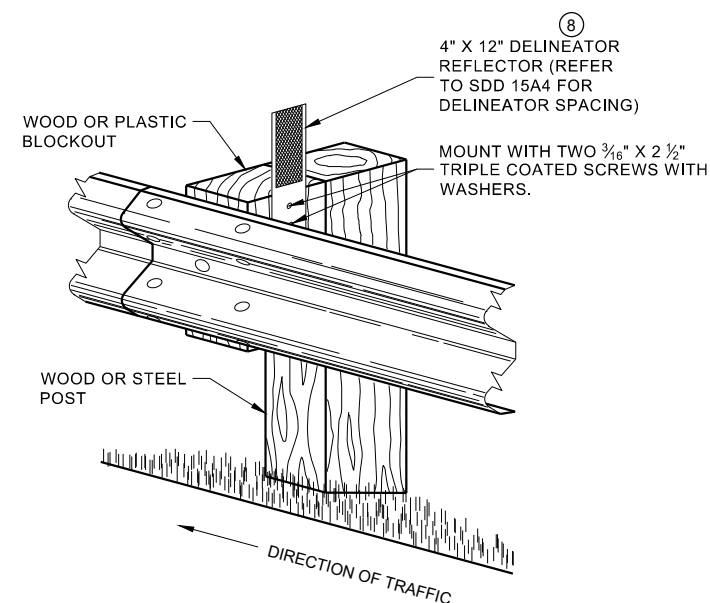
FRONT VIEW AT WOOD POST



SECTION THRU W-BEAM RAIL



**FRONT VIEW
QUARTER POST SPACING (QS)**



**ONE SIDED REFLECTOR DETAIL
AND TYPICAL INSTALLATION**

**MIDWEST GUARDRAIL SYSTEM
(MGS) GUARDRAIL**

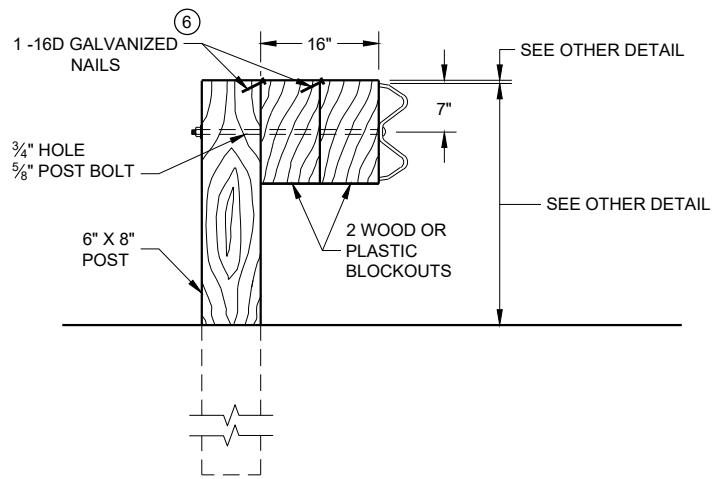
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

6

SDD 14B42 - 07b

SDD 14B42 - 07b

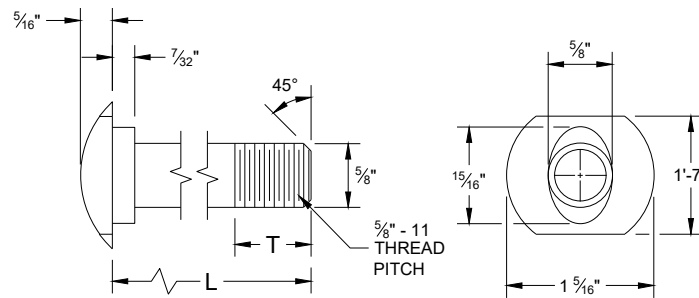


DETAIL FOR 16" BLOCKOUT DEPTH

IT IS ACCEPTABLE TO USE BLOCKOUTS UP TO 16" DEEP TO INCREASE THE POST OFFSET TO AVOID UNDERGROUND OBSTACLES. THERE IS NO LIMIT TO THE NUMBER OF POSTS THAT CAN HAVE ADDITIONAL BLOCKOUTS UP TO 16" DEEP.

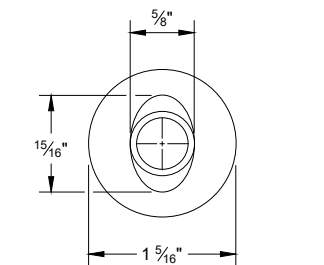
NOTE:

1. ALL FILLETS SHALL HAVE A MINIMUM RADIUS OF 3/16".
2. IF THE BOLT EXTENDS MORE THAN 1/4" FROM THE NUT THE BOLT SHOULD BE TRIMMED BACK.

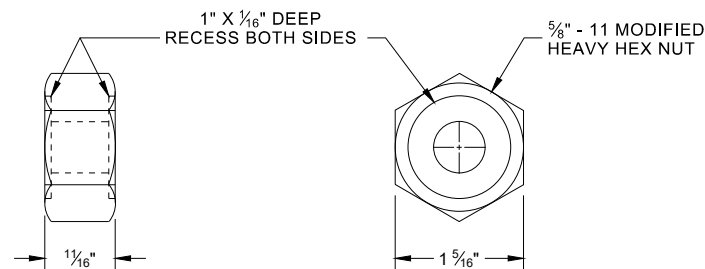


POST BOLT TABLE

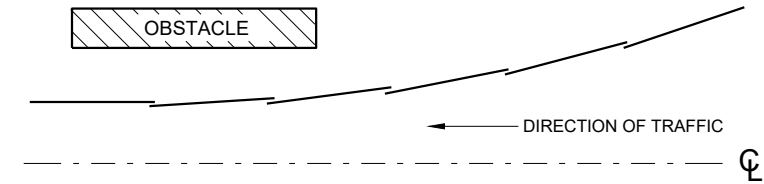
L	T (MIN.)
1 1/4"	1 1/8"
2"	1 3/4"
10"	4"
14"	4 1/16"
18"	4"
21"	4 1/16"
25"	4"



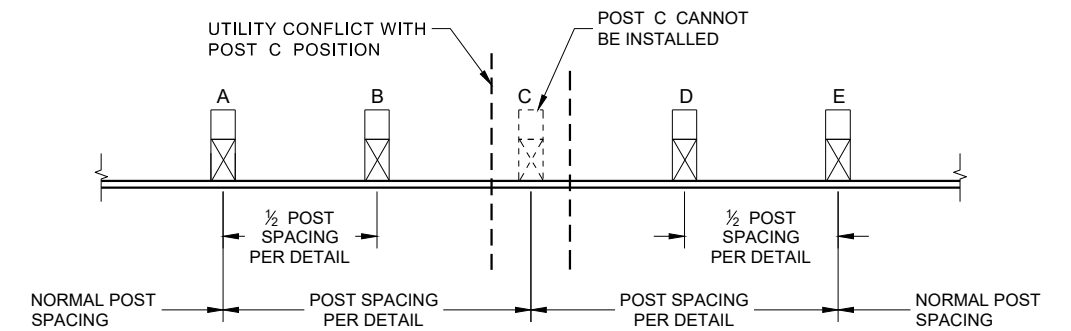
ALTERNATE BOLT HEAD



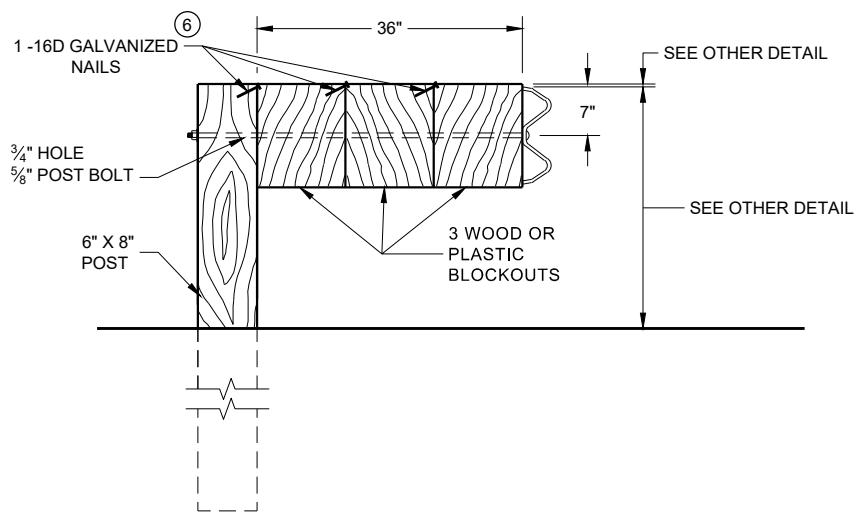
POST BOLT, SPLICE BOLT AND RECESS NUT



PLAN VIEW BEAM LAPPING DETAIL

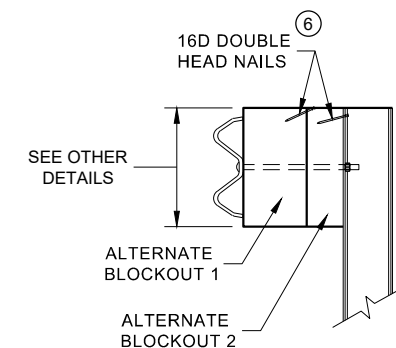


POST DRIVING FOR CONTINUOUS UNDERGROUND OBSTRUCTION

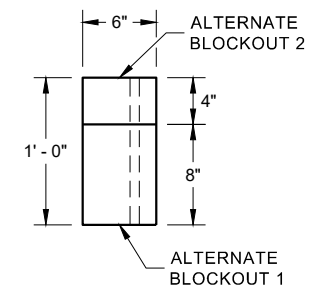


DETAIL FOR 36" BLOCKOUT DEPTH

NOTES: UNDER SPECIAL CIRCUMSTANCES, SUCH AS AVOIDING OBSTACLES THAT ARE NOT RELOCATED, IT IS ACCEPTABLE TO INSTALL ADDITIONAL BLOCKOUTS TO OBTAIN UP TO 36" DEPTH FOR ONE OR TWO POSTS IN A SECTION OF GUARDRAIL. DO NOT USE 16" OR 36" BLOCKOUTS IF IT CAUSES THE POST TO BE DRIVEN BEYOND SHOULDER HINGE POINT OR CAUSES A FIXED OBJECT TO BE WITHIN THE DEFLECTION DISTANCE OF THE BARRIER.



SIDE VIEW



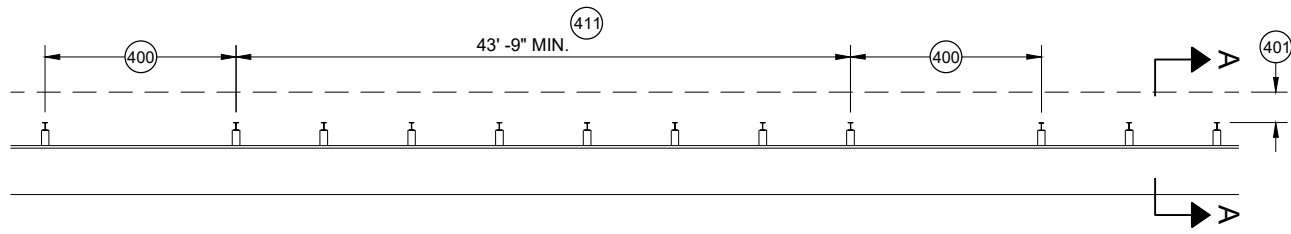
PLAN VIEW

ALTERNATE WOOD BLOCKOUT DETAIL

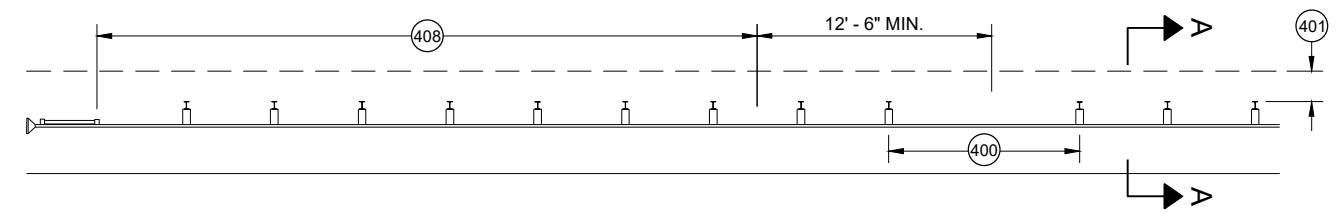
6 WHEN USING STEEL POST AND WOOD BLOCKOUTS, INSTALL FOUR 16D GALVANIZED NAILS. INSTALL NAILS AT THE BACK CORNERS OF THE BLOCK AND BEND THE NAILS OVER THE FLANGE OF THE STEEL POST.

MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL

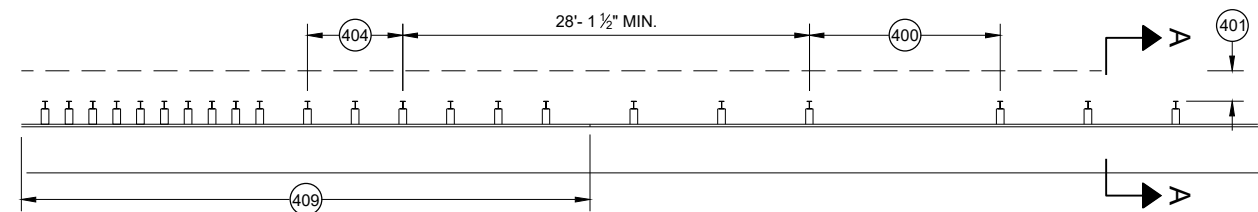
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



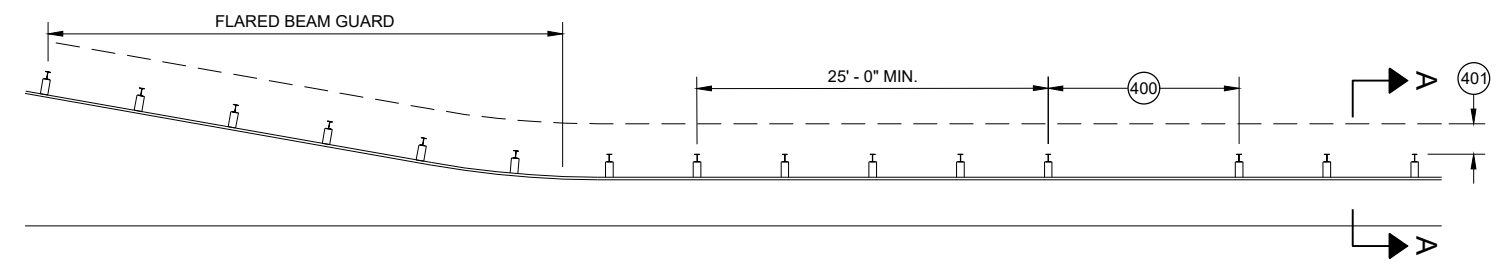
MISSING POST IN MGS GUARDRAIL



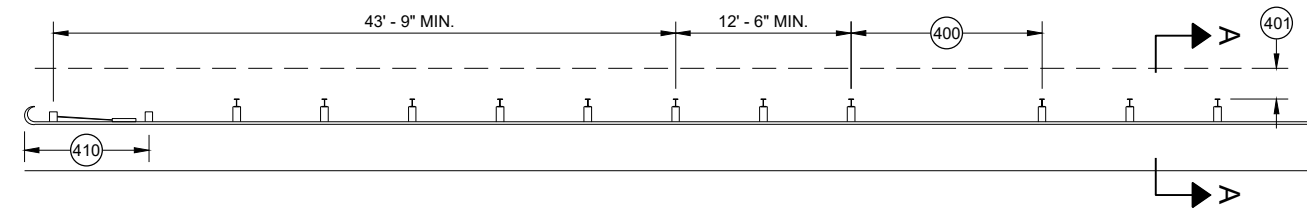
MISSING POST IN MGS GUARDRAIL NEAR EAT



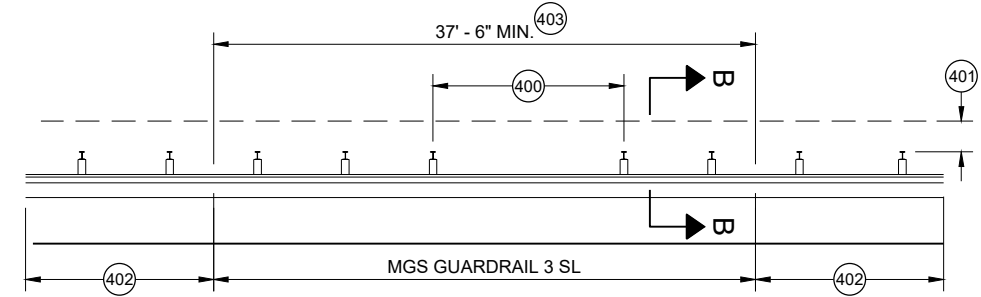
MISSING POST IN MGS GUARDRAIL NEAR AN APPROACH TRANSITION



MISSING POST IN MGS GUARDRAIL NEAR FLARED BEAM GUARD

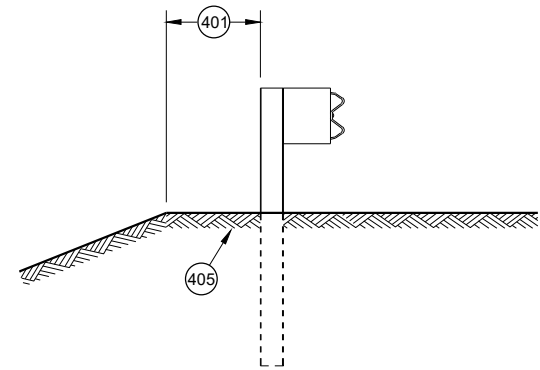


MISSING POST IN MGS GUARDRAIL NEAR A TYPE 2 END TERMINAL

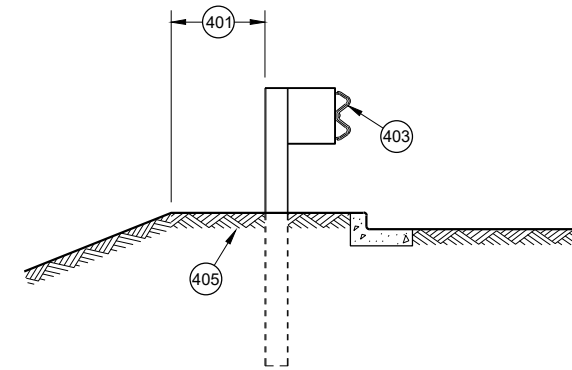


MISSING POST IN SHORT SPAN MGS GUARDRAIL NEAR CURB (SL)

- 400 MAX SPAN 12' - 6"
- 401 2' MIN.
- 402 MGS GUARDRAIL 3
- 403 NESTING BEAM GUARD
- 404 ASYMMETRIC TRANSITION
- 405 SOIL WELL DRAINED AND COMPACTED
- 406 SEE OTHER DRAWINGS IN THIS SDD
- 407 SEE OTHER DRAWINGS FOR MIN. SPACING BETWEEN SPANS
- 408 SEE SDD 14B44
- 409 SEE SDD 14B45
- 410 SEE SDD 14B47
- 411 MINIMUM DISTANCE BETWEEN MISSING POST SPANS.



SECTION A - A



SECTION B - B

MIDWEST GUARDRAIL SYSTEM (MGS) GUARDRAIL	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2021 DATE	/S/ Rodney Taylor ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR
<small>FHWA</small>	

GENERAL NOTES

- (A) THE SLOPE IN THE AREA BOUNDED BY THE GRADELINE, THE HINGE POINT LINE AND THE CLEAR ZONE LIMITS (CZL) SHALL BE 4:1 OR FLATTER.
 - (B) AFTER FINAL ASSEMBLY, RECHECK CABLE TO BE SURE IT IS TAUT AND HAS NOT RELAXED
 - (C) DIFFERENT MANUFACTURERS REQUIRE DIFFERENT PERFORATED W - BEAM RAIL END PANELS. SEE MANUFACTURER'S INFORMATION.
 - (D) ATTACH ALUMINUM SHEET TO E.A.T. HEAD USING 4 STAINLESS STEEL SELF - TAPPING SCREWS. ONE SCREW PER CORNER.
 - (E) HARDWARE MAY VARY BETWEEN MANUFACTURER. SEE MANUFACTURER'S DRAWING FOR INFORMATION.
- DIMENSIONS MAY VARY, MANUFACTURER'S INFORMATION.

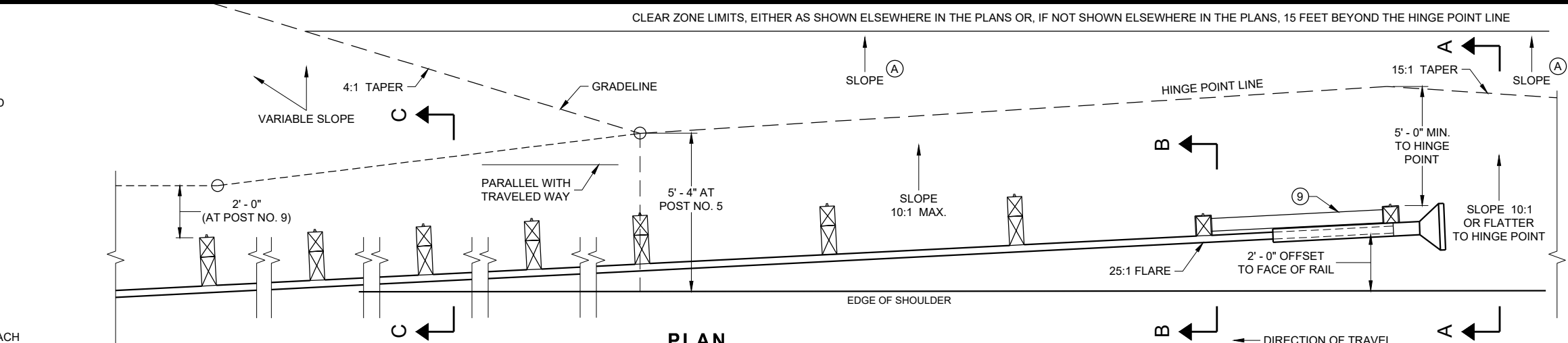
SEE SDD 14B42 FOR MORE INFORMATION.

* DO NOT ATTACH BLOCKOUTS TO POST 1 AND 2.

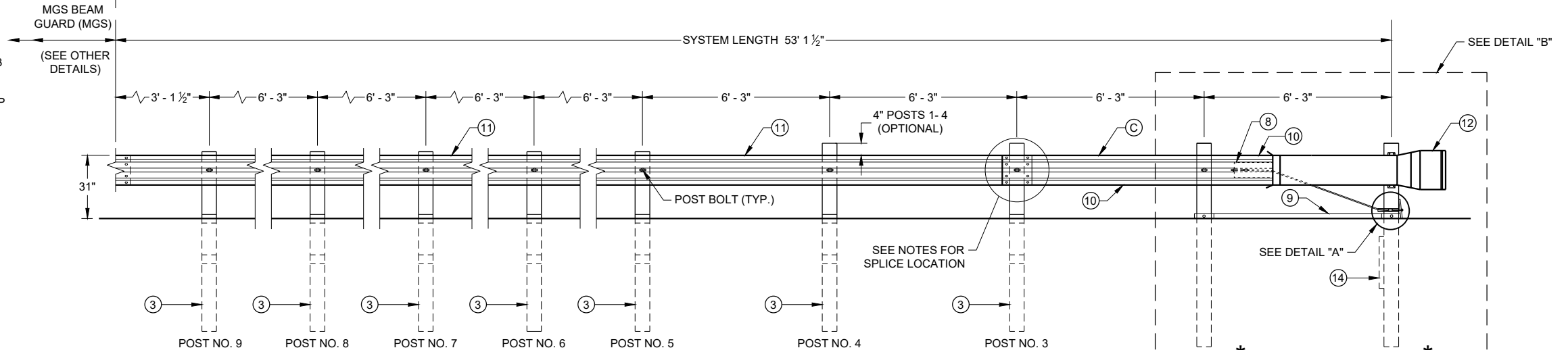
DO NOT INSTALL REFLECTORS ON THE FIRST 50 FEET OF THE APPROACH END OF THE ENERGY ABSORBING TERMINAL.

SEE MANUFACTURER'S DRAWING FOR SPLICE LOCATION, HARDWARE DIMENSIONS AND INSTALLATION INSTRUCTIONS.

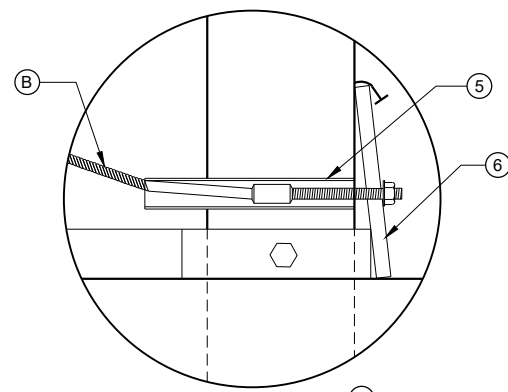
THE CENTER OF THE UPPER 3 1/2" DIAMETER HOLE ON POST NUMBER 3 THROUGH POST 9 IS TO BE FLUSH WITH THE GROUND LINE UP TO A MAXIMUM OF 2" ABOVE GROUND LINE. WOOD BLOCKS ON POSTS NUMBERED 3 THROUGH 9 MAY BE ADJUSTED UP TO 3" ABOVE THE TOP OF POST.



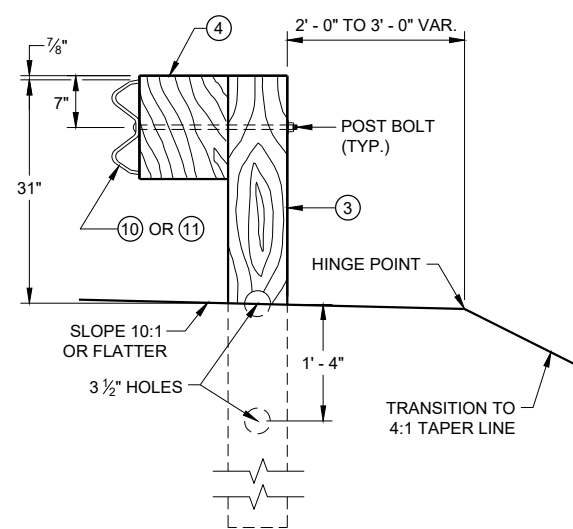
PLAN



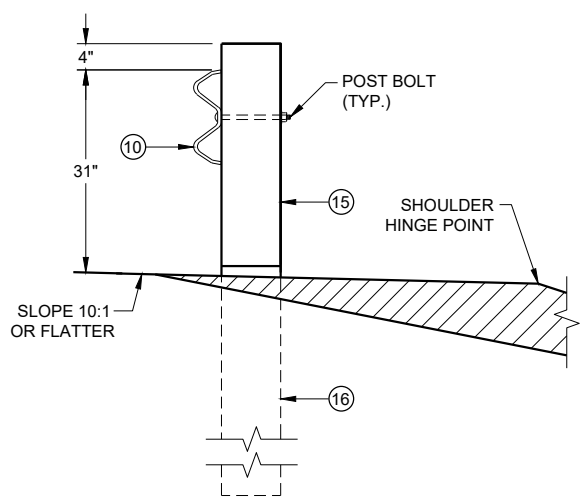
ELEVATION



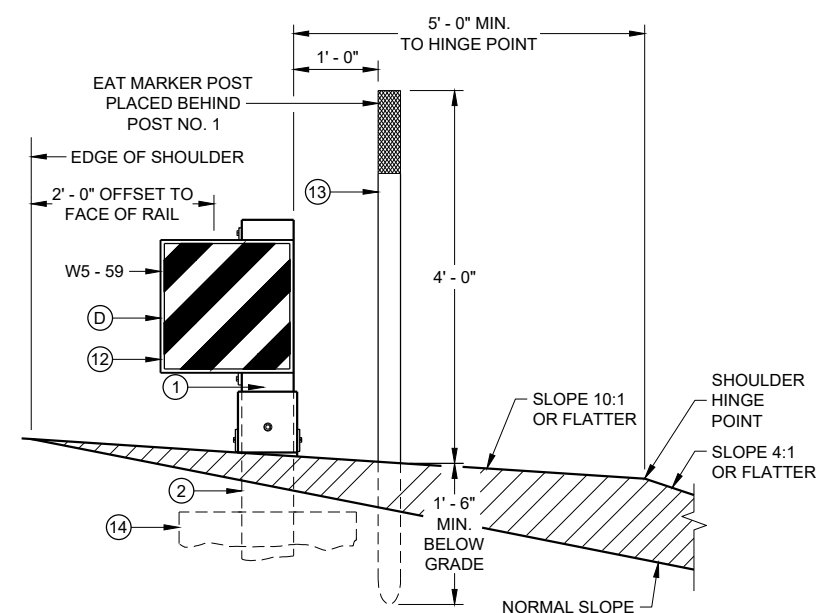
DETAIL "A"



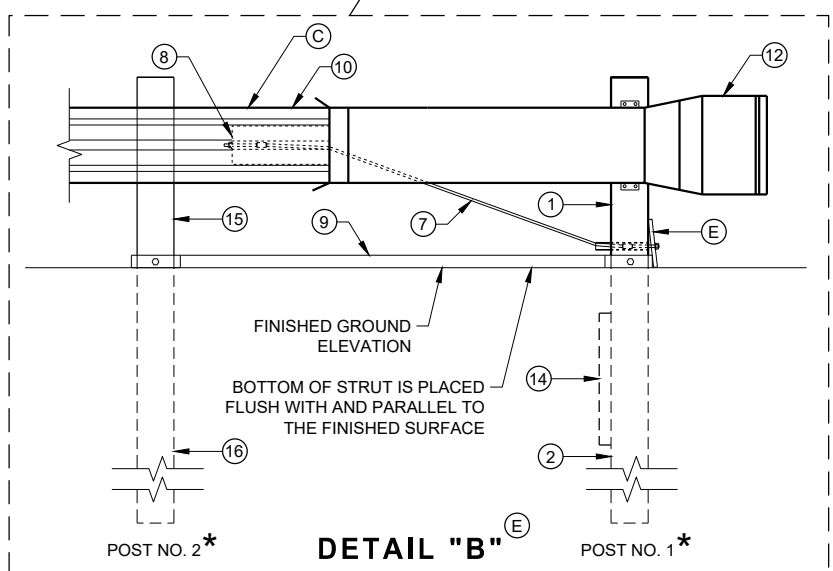
**SECTION C - C
TYPICAL AT POST NOS. 3 - 9**



**SECTION B - B
TYPICAL AT POST NO. 2***



**SECTION A - A
TYPICAL AT POST NO. 1***



DETAIL "B"

**MIDWEST GUARDRAIL SYSTEM
ENERGY ABSORBING TERMINAL
(MGS)**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

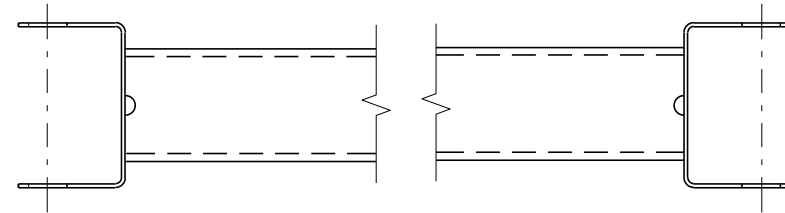
6

SDD 14B44 - 04a

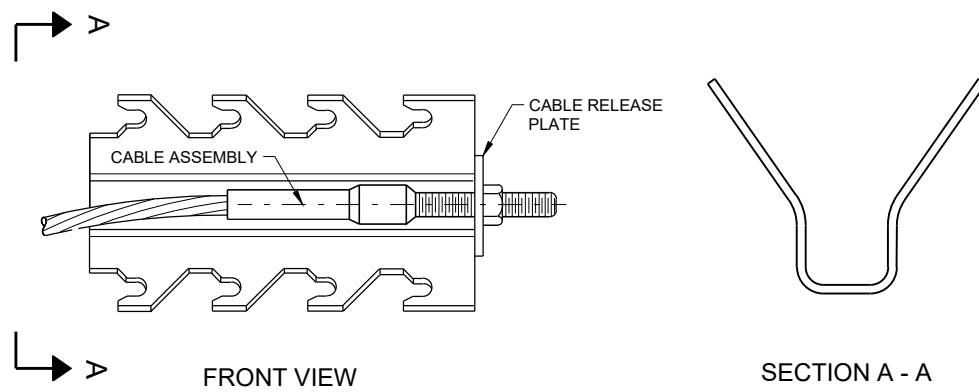
SDD 14B44 - 04a

BILL OF MATERIALS

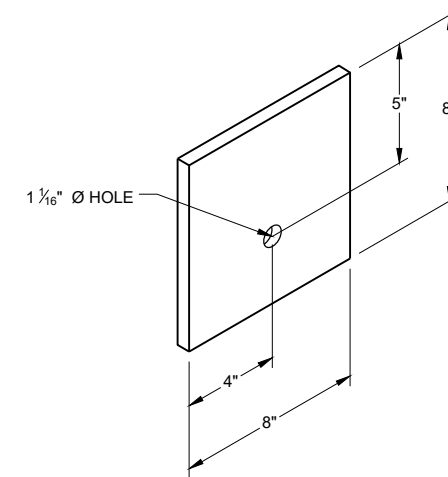
PART NO.	DESCRIPTION MATERIALS PROVIDED BY MGS EAT MANUFACTURER. SEE MANUFACTURER'S DETAILS FOR MORE INFORMATION.
①	UPPER POST NO. 1 6" X 6" TUBE
②	LOWER POST NO. 1
③	WOOD CRT
④	WOOD BLOCKOUT
⑤	PIPE SLEEVE
⑥	BEARING PLATE
⑦	BCT CABLE ASSEMBLY
⑧	ANCHOR CABLE BOX
⑨	GROUND STRUT
⑩	PERFORATED W-BEAM RAIL END PANEL, 12'-6" LONG.
⑪	STANDARD W-BEAM RAIL. MULTIPLE SECTIONS REQUIRED. SECTIONS VARY IN LENGTH.
⑫	IMPACT HEAD
⑬	EAT MARKER POST - YELLOW (SEE APPROVED PRODUCTS LIST)
⑭	SOIL PLATE
⑮	UPPER POST NO. 2
⑯	LOWER POST NO. 2



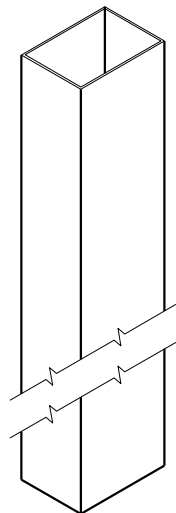
GENERIC GROUND STRUT ⑨ ⑤



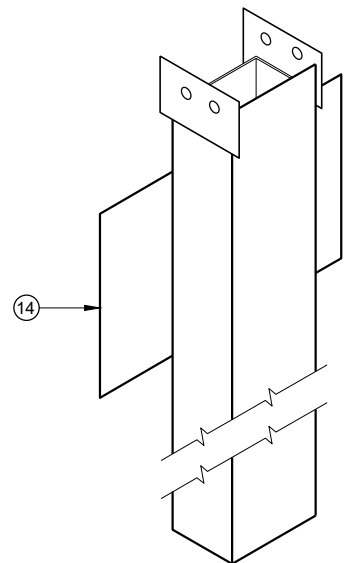
GENERIC ANCHOR CABLE BOX ⑨ ⑤



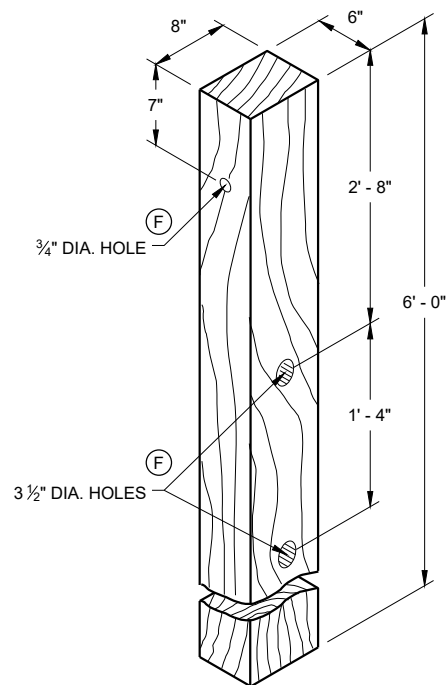
BEARING PLATE ⑥ ⑤



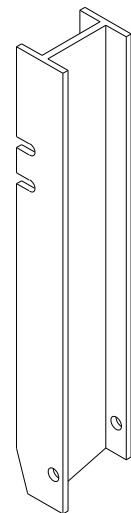
UPPER POST NO. 1 ^① (E)



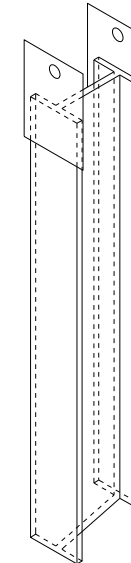
LOWER POST NO. 1 ^② (E)



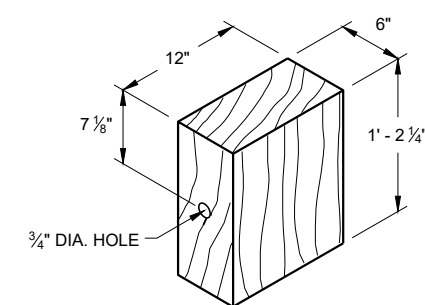
WOOD CRT POST ^③ (E)
POSTS NUMBER 3-9



UPPER POST NO. 2 ^⑮ (E)

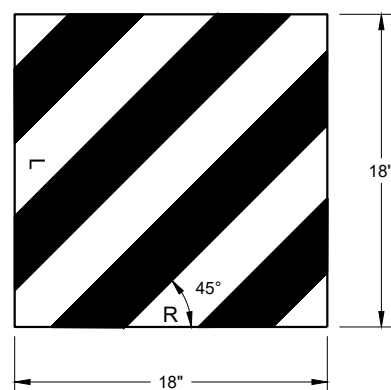


LOWER POST NO. 2 ^⑯ (E)



WOOD BLOCKOUT ^④
REQ'D. AT ALL POSTS EXCEPT POST NO'S 1 & 2

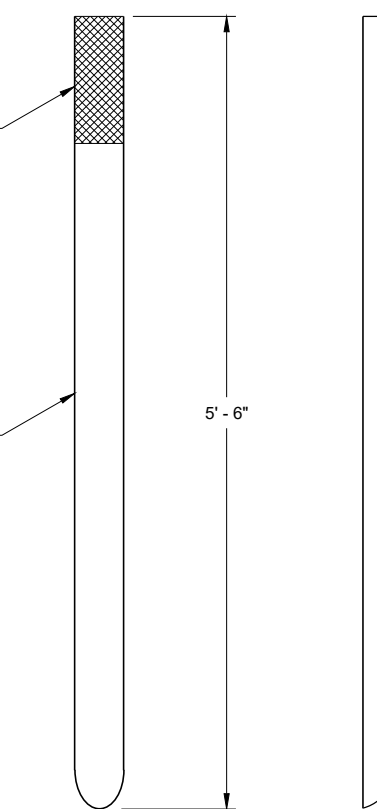
6



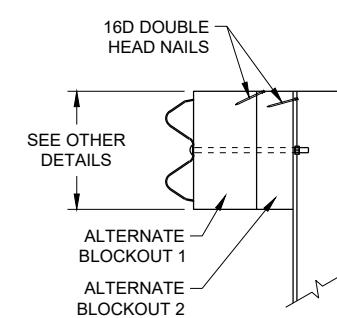
REFLECTIVE SHEETING DETAIL ^⑤

TYPE H
YELLOW REFLECTIVE
SHEETING 3" X 9".
SEE STANDARD
SPECIFICATION 637.

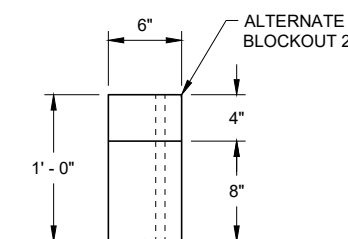
E.A.T. MARKER
POST (YELLOW)



FRONT VIEW SIDE VIEW
E.A.T. MARKER POST ^⑬



SIDE VIEW



TOP VIEW

ALTERNATE WOOD
BLOCKOUT DETAIL

6

SDD 14B44 - 04c

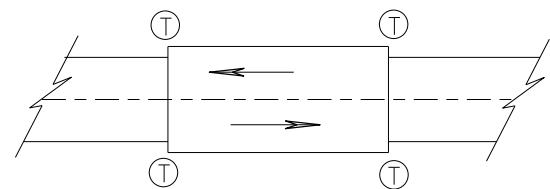
SDD 14B44 - 04c

**MIDWEST GUARDRAIL SYSTEM
ENERGY ABSORBING TERMINAL
(MGS)**

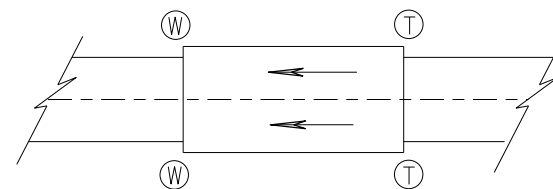
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
7/2018 DATE /S/ Rodney Taylor
ROADWAY STANDARDS DEVELOPMENT
UNIT SUPERVISOR

FHWA



TWO WAY TRAFFIC



ONE WAY TRAFFIC

(T) THRIE BEAM CONNECTION

(W) W-BEAM CONNECTION WHEN REQUIRED

TYPICAL LOCATIONS OF THRIE BEAM AND W-BEAM CONNECTIONS TO BRIDGE

GENERAL NOTES

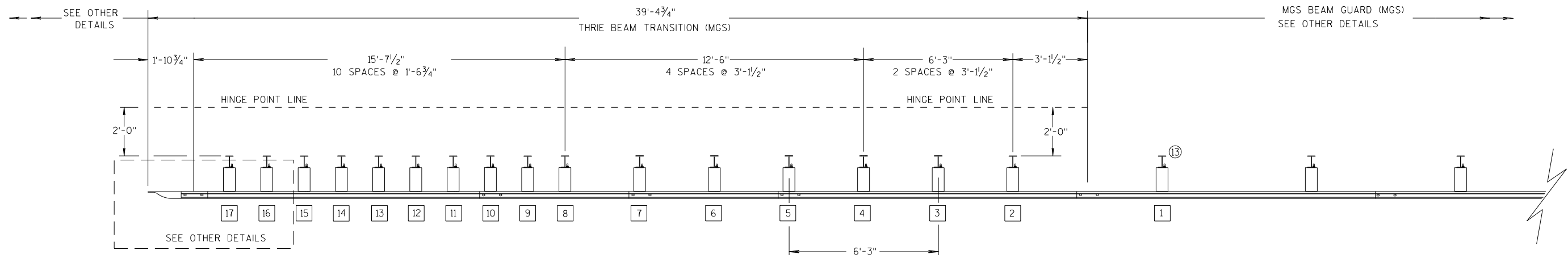
IF ROCK IS ENCOUNTERED, REMOVE ROCK TO FULL DEPTH OF POST PLUS 2 1/2", AND 12" DIAMETER AROUND POST. SEE 14B42 FOR MORE DETAILS.

TRANSITION USES STEEL POSTS ONLY.

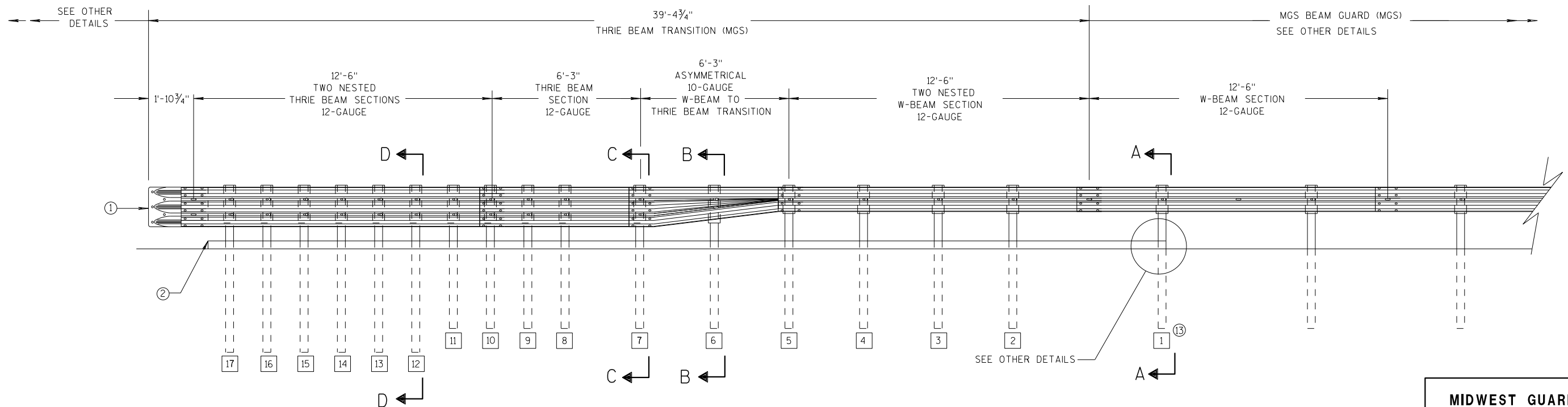
SEE STANDARD DETAIL DRAWING 14 B 42 FOR MORE INFORMATION.

POST 2 THROUGH 17 USES STEEL POST ONLY

- ① BRIDGE RAILING TYPE "W" DOES NOT REQUIRE A TERMINAL CONNECTOR.
- ② OPTIONAL CURB AND GUTTER OR DRAINAGE FEATURE SEE PLAN FOR INFORMATION.
- ⑬ STEEL OR WOOD POST IS ACCEPTABLE AT POST 1. SEE SDD14B42



PLAN VIEW



ELEVATION VIEW

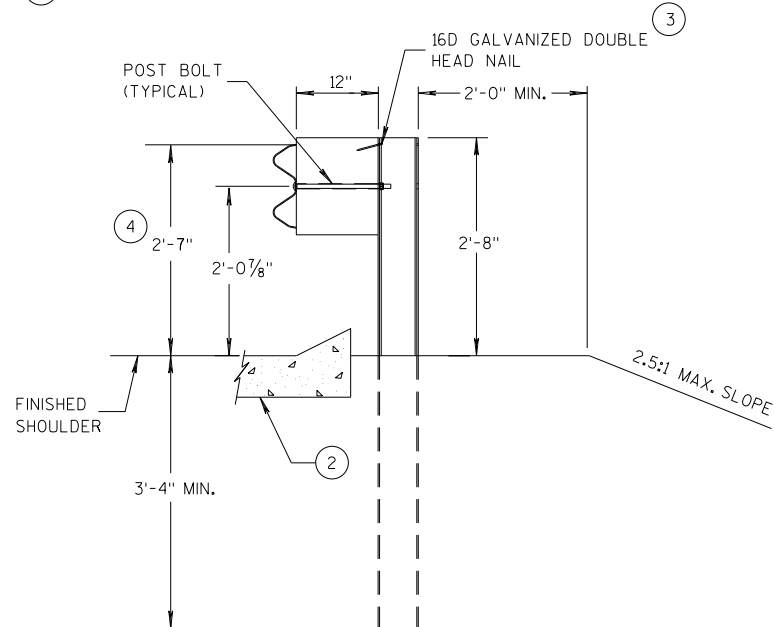
MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION

**MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)**

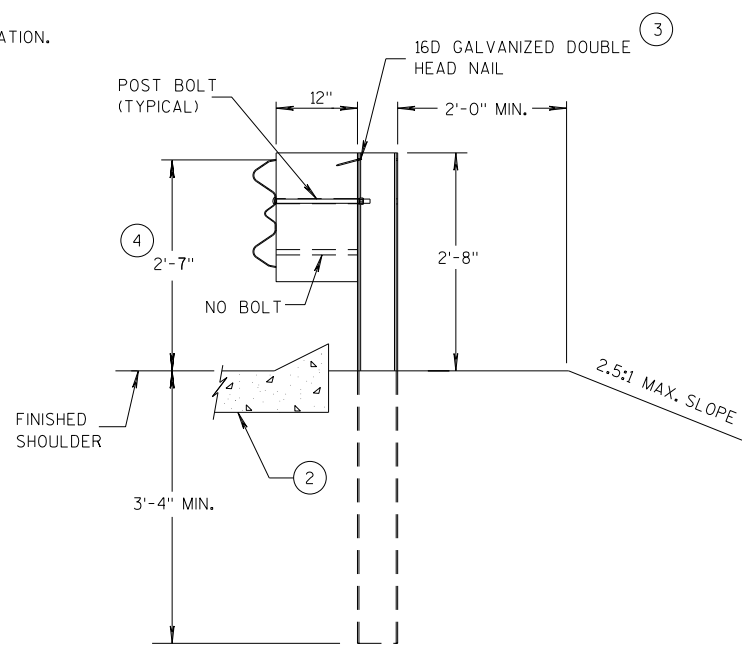
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

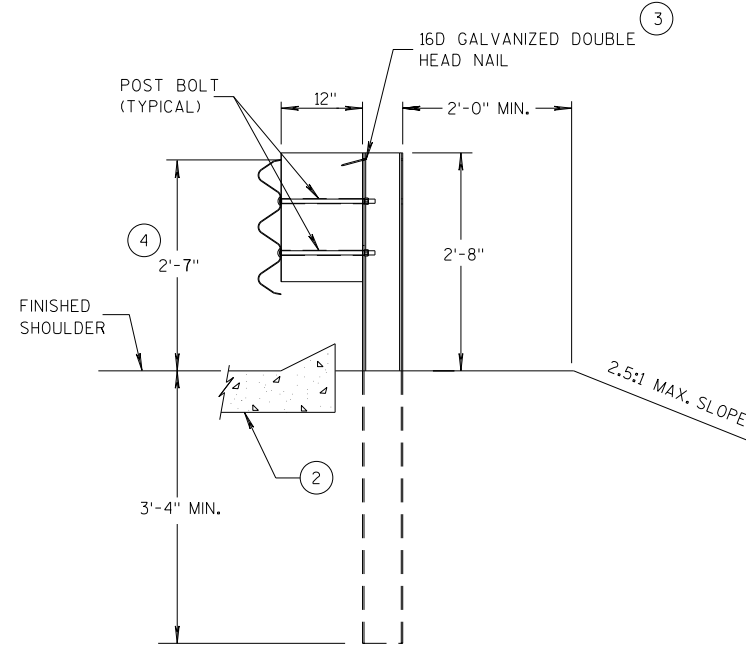
- ② OPTIONAL CURB AND GUTTER OR DRAINAGE FEATURE SEE PLAN FOR INFORMATION.
- ③ WHEN USING STEEL POSTS AND WOOD BLOCKOUTS INSTALL FOUR 10D GALVANIZED NAILS. INSTALL NAILS AT THE BACK CORNERS OF THE BLOCK AND BEND THE NAILS OVER THE FLANGE OF THE STEEL POST.
- ④ TOLERANCE FOR TOP OF W-BEAM RAIL IS ± 1".
- ⑬ STEEL OR WOOD POST IS ACCEPTABLE AT POST 1. SEE SDD 14B42



**SECTION A-A
POSTS 1-5**



**SECTION B-B
POST 6**



**SECTION C-C
POSTS 7-11**

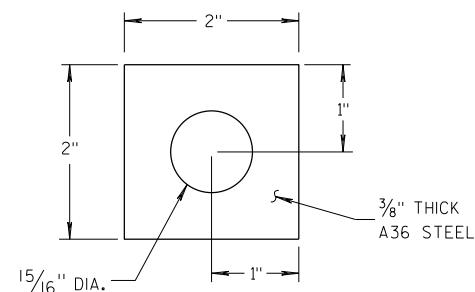
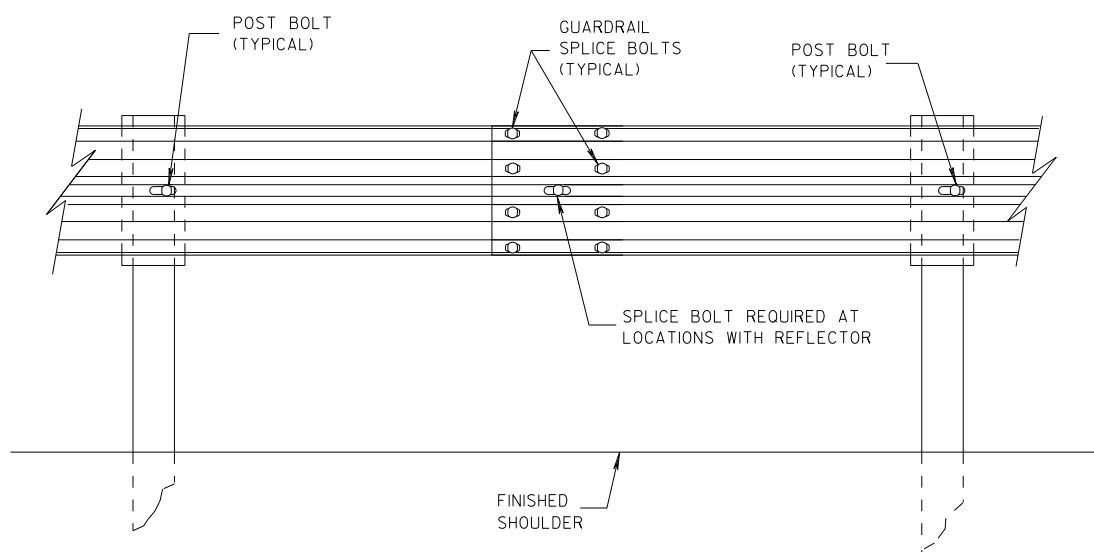
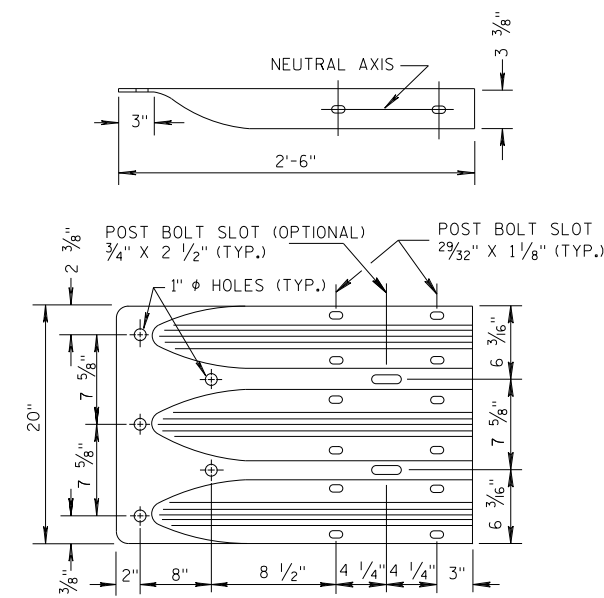


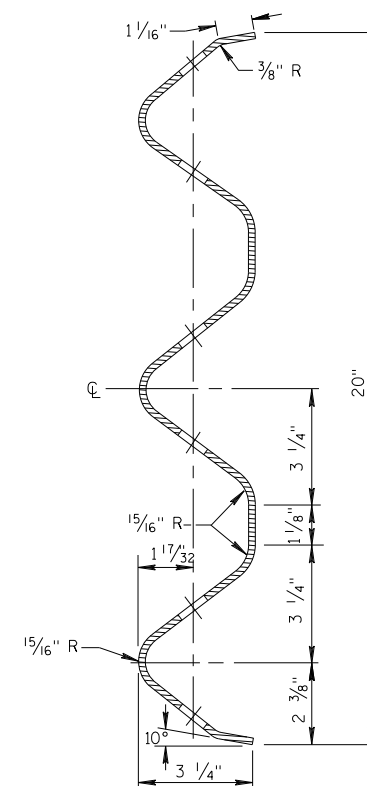
PLATE WASHER DETAIL



SPLICE DETAIL



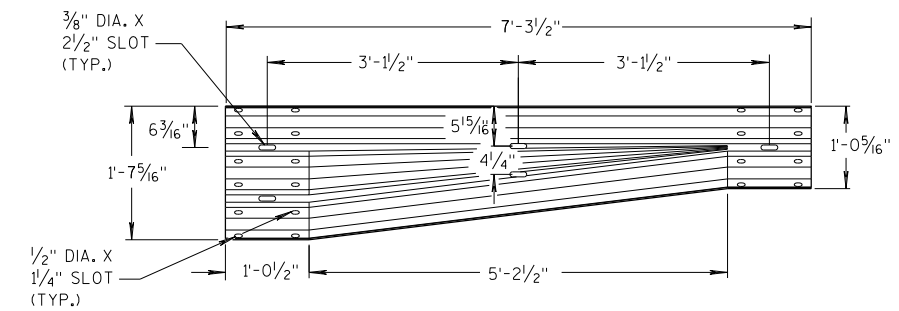
**THRIE BEAM
TERMINAL CONNECTOR**



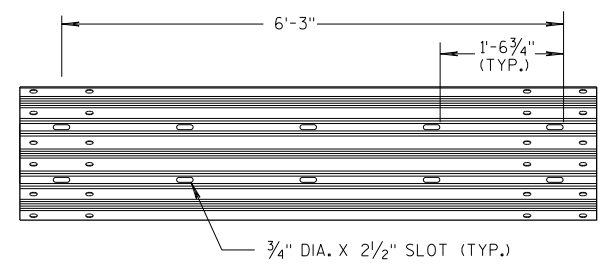
**SECTION THRU THRIE
BEAM RAIL ELEMENT**

**MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)**

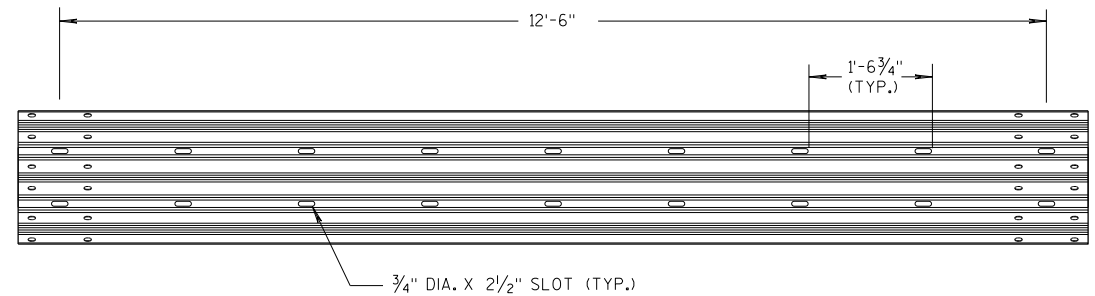
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



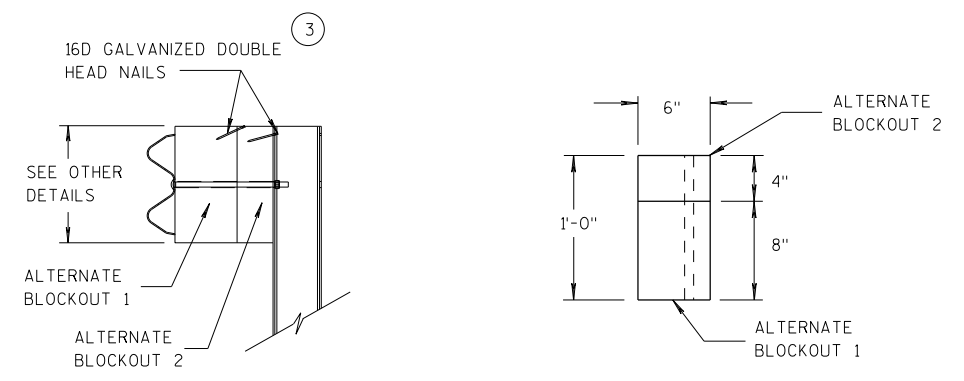
W-BEAM TO THRIE BEAM TRANSITION SECTION



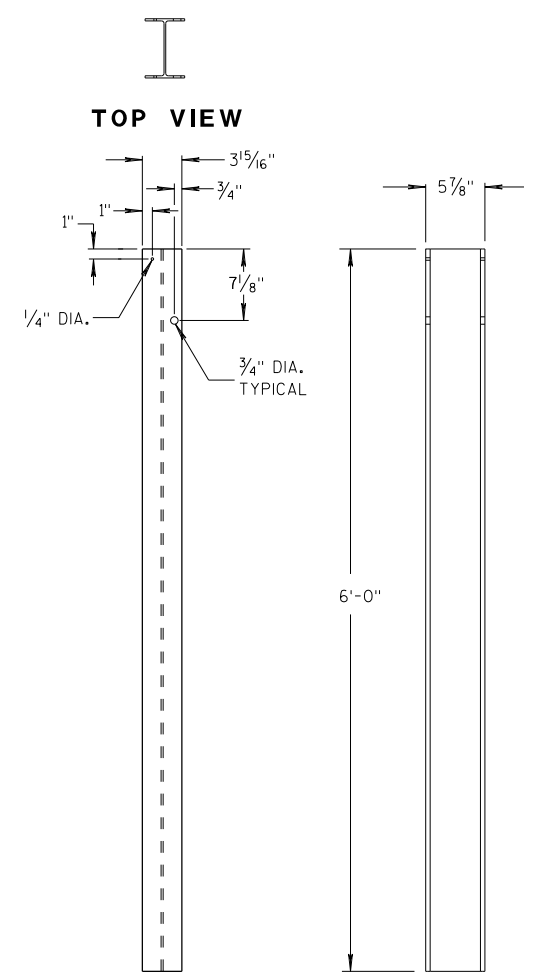
6'-3\"/>



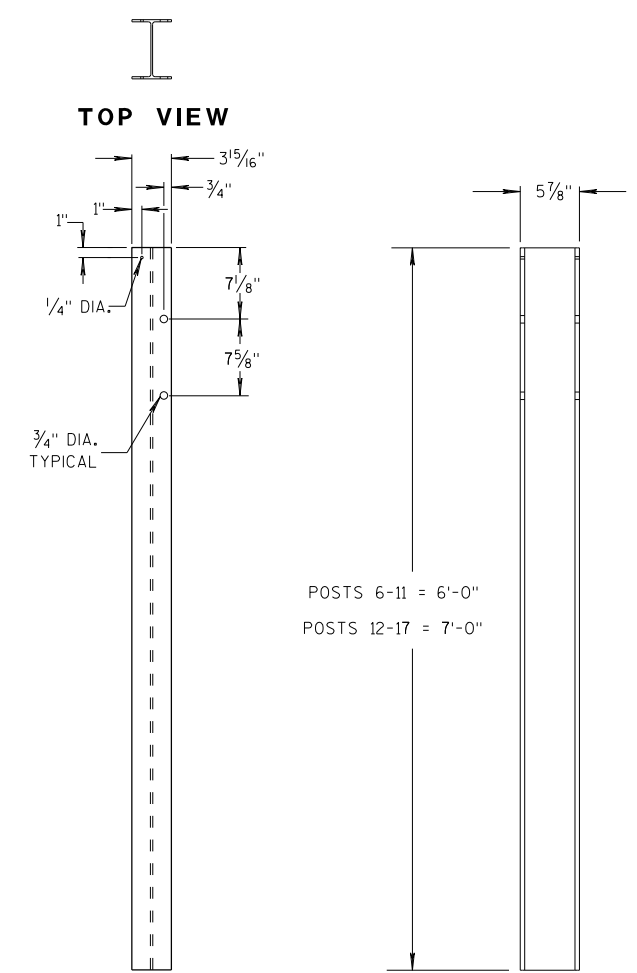
12'-6\"/>



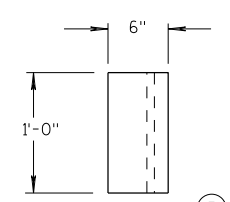
ALTERNATE WOOD BLOCKOUT DETAIL



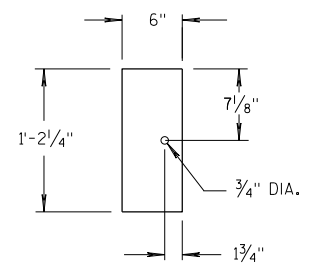
STEEL POSTS 1-5



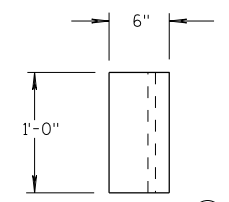
STEEL POSTS 6-17



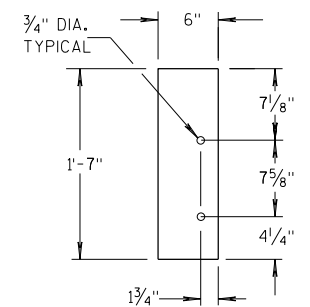
TOP VIEW



BLOCKOUT POSTS 1-5



TOP VIEW



BLOCKOUT POSTS 6-17

GENERAL NOTES

- STEEL POSTS ARE W6X9 OR W6X8.5.
- BOLT HOLES FOR POST ARE ON FRONT AND OF SIDE OF POST.
- (3) WHEN USING STEEL POSTS AND WOOD BLOCKOUTS INSTALL FOUR 16D GALVANIZED NAILS. INSTALL NAILS AT THE BACK CORNERS OF THE BLOCK AND BEND THE NAILS OVER THE FLANGE OF THE STEEL POST.
- (5) WOOD BLOCKS MAY BE CONSTRUCTED OUT OF 2 WOOD BLOCKS. SEE ALTERNATE WOOD BLOCK DETAIL.
- (13) STEEL OR WOOD POST IS ACCEPTABLE AT POST 1. SEE SDD 14B42.

**MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)**

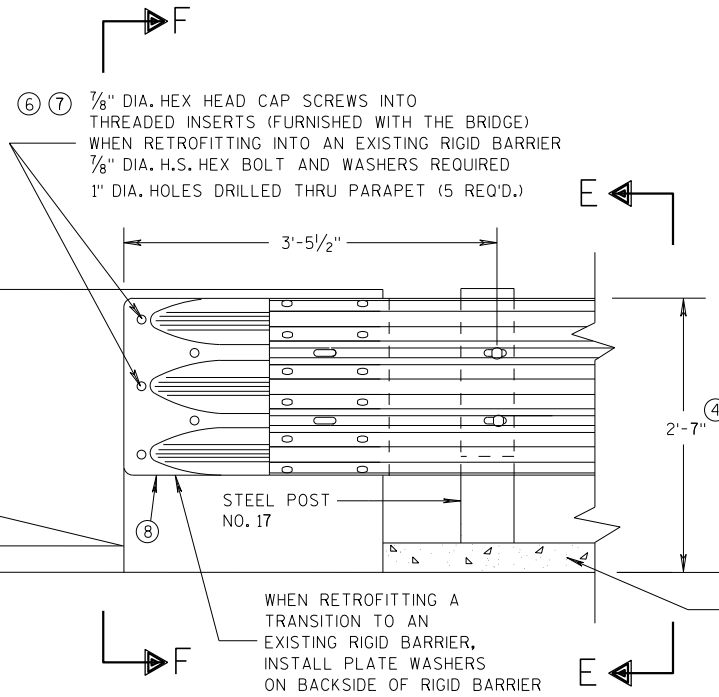
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

6

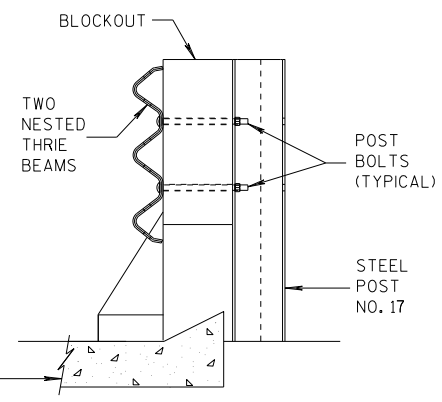
S.D.D. 14 B 45-5c

S.D.D. 14 B 45-5c



FRONT VIEW

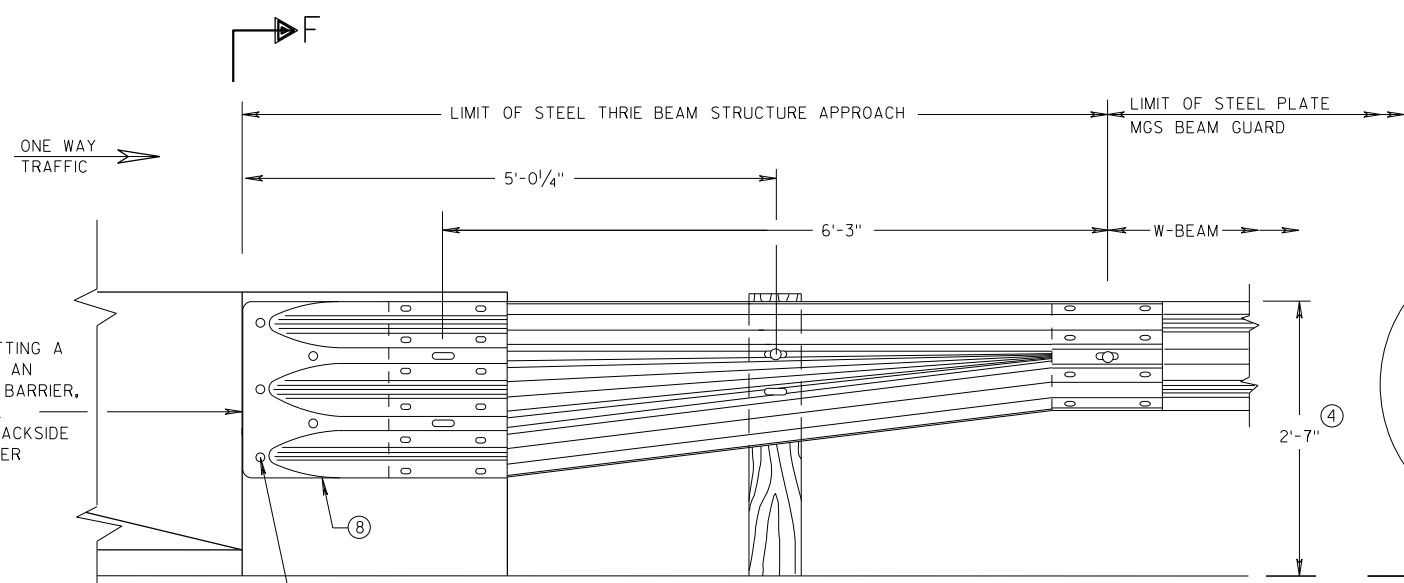
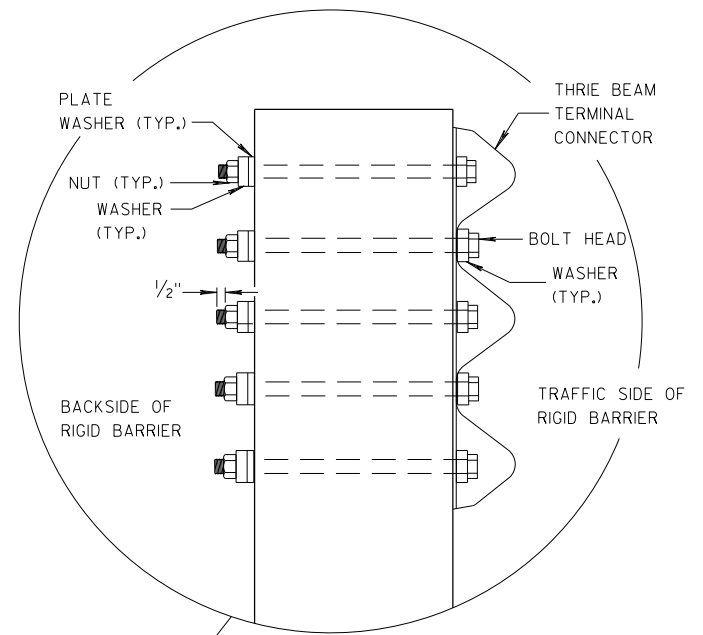
THRIE BEAM CONNECTION TO BRIDGE PARAPET WITH SQUARE ENDS



SECTION E-E

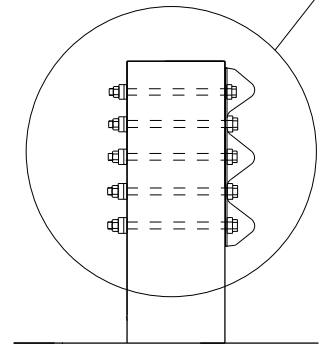
GENERAL NOTES

- THESE ARE TYPICAL CONNECTION DETAILS. ADJUST THE POSITION OF CONNECTIONS TO EXISTING BRIDGES TO FIT THE ACTUAL BRIDGE AND SITE DIMENSIONS.
- (2) OPTIONAL CURB AND GUTTER OR DRAINAGE FEATURE SEE PLAN FOR INFORMATION.
- (4) TOLERANCE FOR TOP OF BEAM IS $\pm 1"$.
- (6) DRILLING BOLT HOLES THROUGH THE PARAPET, BOLTS, NUTS, WASHERS AND REPAIRING DAMAGED CONCRETE ARE INCIDENTAL TO THE CONTRACT.
- (7) BOLTS MAY BE A325 BOLTS OR A449 BOLTS. BOLT LENGTH AND THREADING LENGTH ARE TO ALLOW FOR A TIGHT CONNECTION BETWEEN RIGID BARRIER AND THRIE BEAM CONNECTION PLATE. CONTRACTOR IS TO FIELD VERIFY BOLT LENGTH AND THREAD LENGTH. ONE ROUND WASHER REQUIRED BETWEEN BOLT HEAD AND THRIE BEAM CONNECTOR PLATE. BOLTS THAT EXTEND THROUGH THE PARAPET AND OUT THE BACK FACE REQUIRE A HARDENED ROUND STEEL WASHER THAT IS 2" O.D. X 5/32" THICK AND ONE PLATE WASHER. REPAIR ANY DAMAGED CONCRETE FROM BOLT INSTALLATION.
- (8) THE RECESS FOR A W-BEAM CONNECTION, WHICH EXISTS ON SOME PARAPETS OF THIS TYPE, SHALL BE FILLED WITH A TREATED TIMBER BLOCKOUT. BLOCKOUT SIZE IS 1'-6" X 2'-0" X 3 1/2".

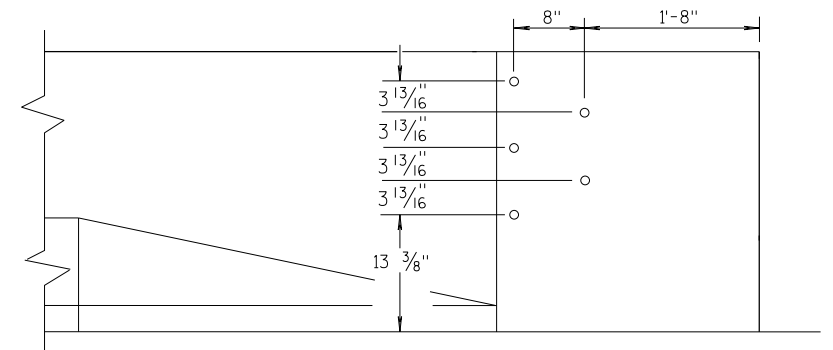


FRONT VIEW

**W BEAM TRANSITION AND CONNECTION TO BRIDGE PARAPETS WITH SQUARE ENDS
(USE ONLY ON THE TRAFFIC EXIT END OF ONE WAY BRIDGES)**



SECTION F-F



DRILL HOLE LOCATION

6

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S.D.D. 14 B 45-5d

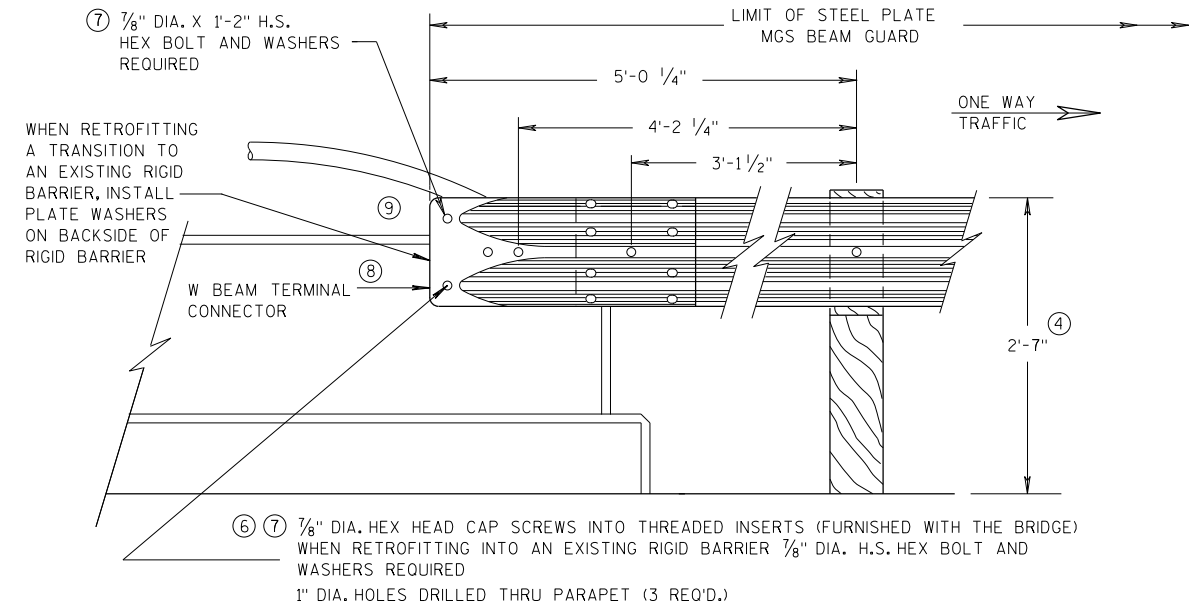
S.D.D. 14 B 45-5d

MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 07/2018 DATE	/S/ Rodney Taylor ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR
FHWA	

GENERAL NOTES

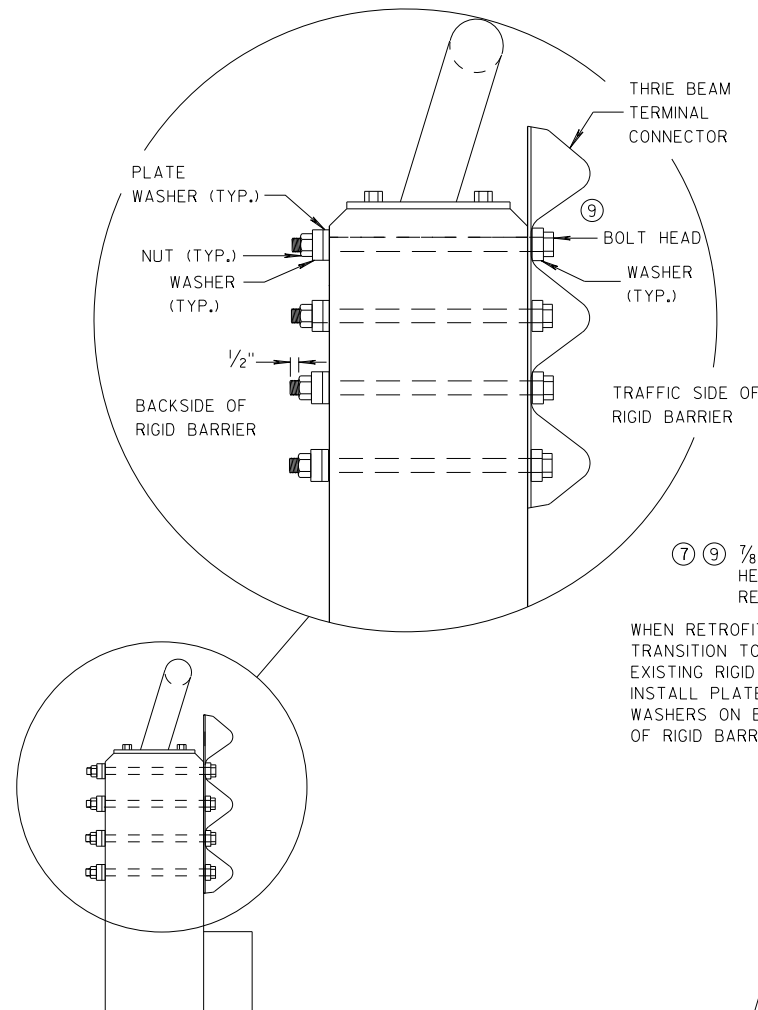
THESE ARE TYPICAL CONNECTION DETAILS. ADJUST THE POSITION OF CONNECTIONS TO EXISTING BRIDGES TO FIT THE ACTUAL BRIDGE AND SITE DIMENSIONS.

- ② OPTIONAL CURB AND GUTTER OR DRAINAGE FEATURE SEE PLAN FOR INFORMATION.
- ④ TOLERANCE FOR TOP OF BEAM IS $\pm 1"$.
- ⑥ DRILLING BOLT HOLES THROUGH THE PARAPET, BOLTS, NUTS, WASHERS AND REPAIRING DAMAGED CONCRETE ARE INCIDENTAL TO THE CONTRACT.
- ⑦ BOLTS MAY BE A325 BOLTS OR A449 BOLTS. BOLT LENGTH AND THREADING LENGTH ARE TO ALLOW FOR A TIGHT CONNECTION BETWEEN RIGID BARRIER AND THRIE BEAM CONNECTION PLATE. CONTRACTOR IS TO FIELD VERIFY BOLT LENGTH AND THREAD LENGTH. ONE ROUND WASHER REQUIRED BETWEEN BOLT HEAD AND THRIE BEAM CONNECTOR PLATE. BOLTS THAT EXTEND THROUGH THE PARAPET AND OUT THE BACK FACE REQUIRE A HARDENED ROUND STEEL WASHER THAT IS 2" O.D. X 5/32" THICK AND ONE PLATE WASHER. REPAIR ANY DAMAGED CONCRETE FROM BOLT INSTALLATION.
- ⑧ THE RECESS FOR A W-BEAM CONNECTION, WHICH EXISTS ON SOME PARAPETS OF THIS TYPE, SHALL BE FILLED WITH A TREATED TIMBER BLOCKOUT. BLOCKOUT SIZE IS 1'-6" X 2'-0" X 3 1/2".
- ⑨ BOLT, NUT AND WASHERS NOT REQUIRED FOR THIS LOCATION WHEN RETROFITTING AN EXISTING PAPAPET AND THE HOLE IS EITHER ABOVE PARAPET OR WITHIN 4 INCHES OF THE EDGE OF PARAPET.

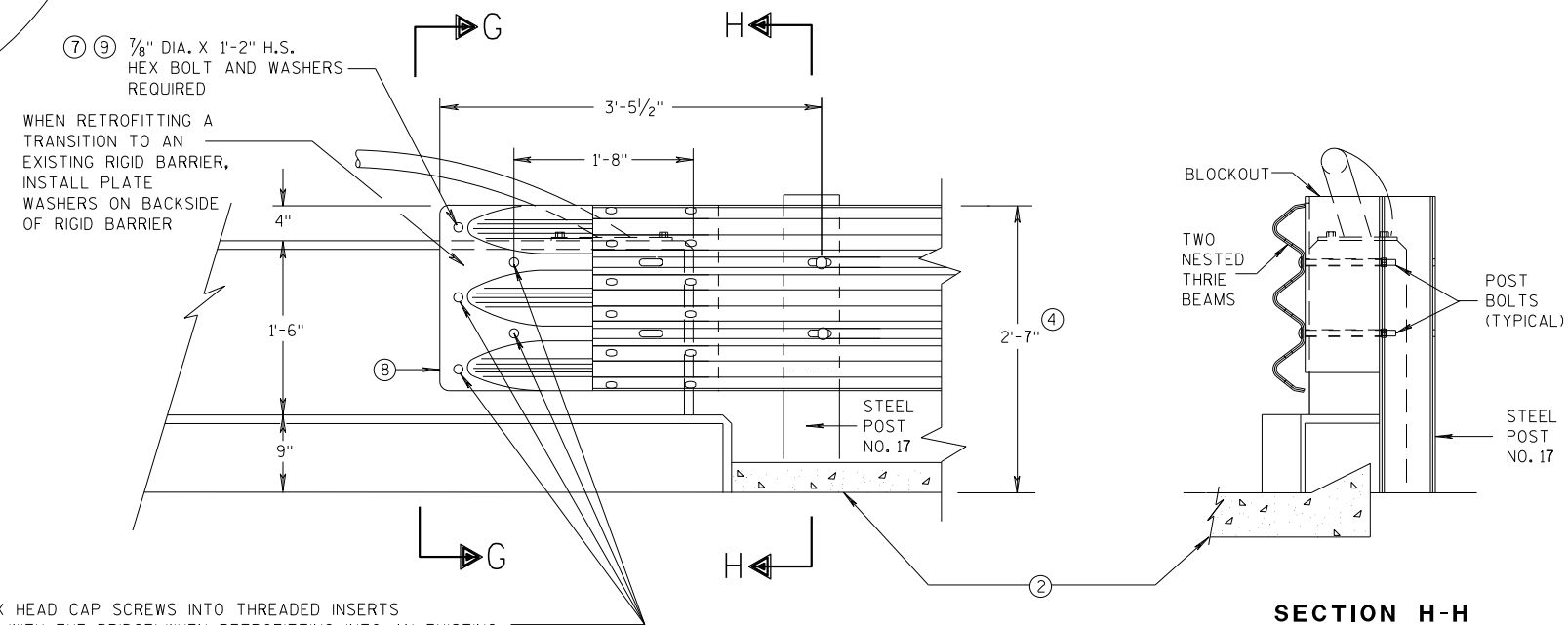


FRONT VIEW

W BEAM CONNECTION TO VERTICAL FACE PARAPET (USE ONLY ON THE TRAFFIC EXIT END OF ONE WAY BRIDGES)



SECTION G-G



FRONT VIEW

THRIE BEAM CONNECTION TO VERTICAL FACED PARAPETS

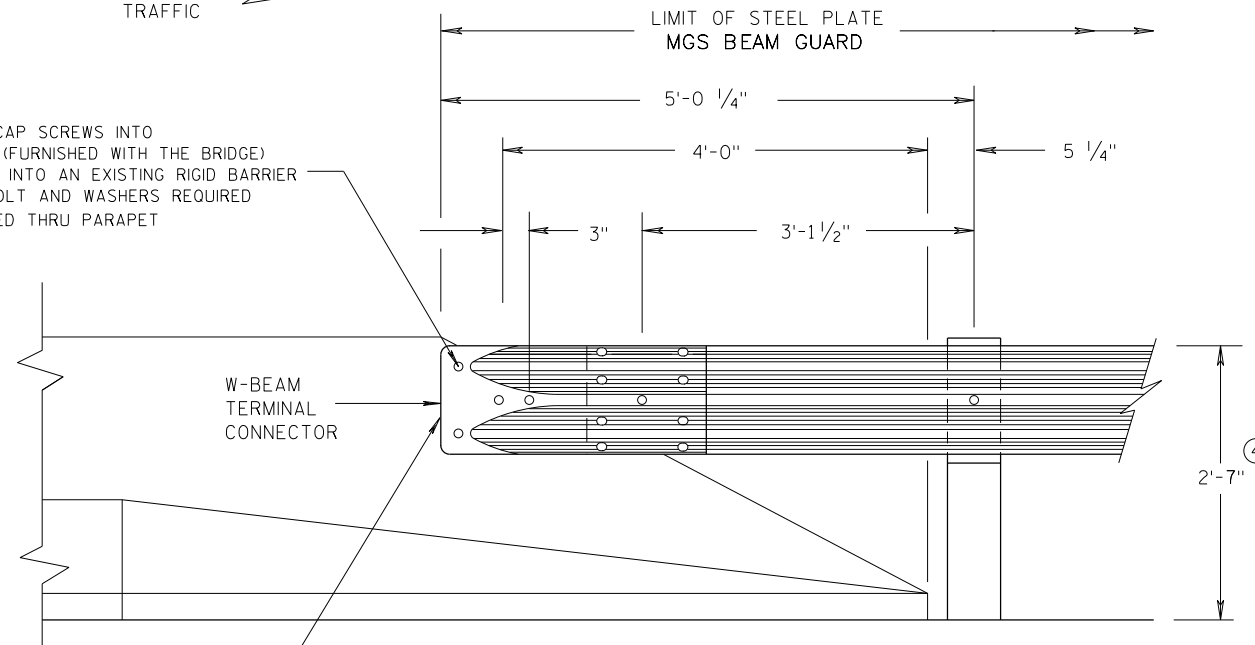
MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
07/2018 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
FHWA UNIT SUPERVISOR

ONE WAY
TRAFFIC

⑥ ⑦ 7/8" DIA. HEX HEAD CAP SCREWS INTO
THREADED INSERTS (FURNISHED WITH THE BRIDGE)
WHEN RETROFITTING INTO AN EXISTING RIGID BARRIER
7/8" DIA. H.S. HEX BOLT AND WASHERS REQUIRED
1" DIA. HOLES DRILLED THRU PARAPET
(4 REQ'D.)



W-BEAM
TERMINAL
CONNECTOR

WHEN RETROFITTING A TRANSITION
TO AN EXISTING RIGID BARRIER,
INSTALL PLATE WASHERS ON
BACKSIDE OF RIGID BARRIER.

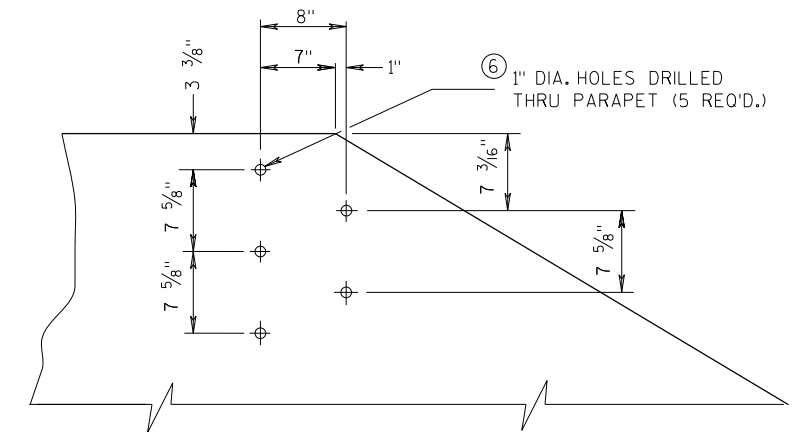
FRONT VIEW

**W BEAM CONNECTION TO
PARAPETS WITH SLOPED ENDS**

(USE ONLY AT TRAFFIC EXIT END OF ONE WAY BRIDGE)

GENERAL NOTES

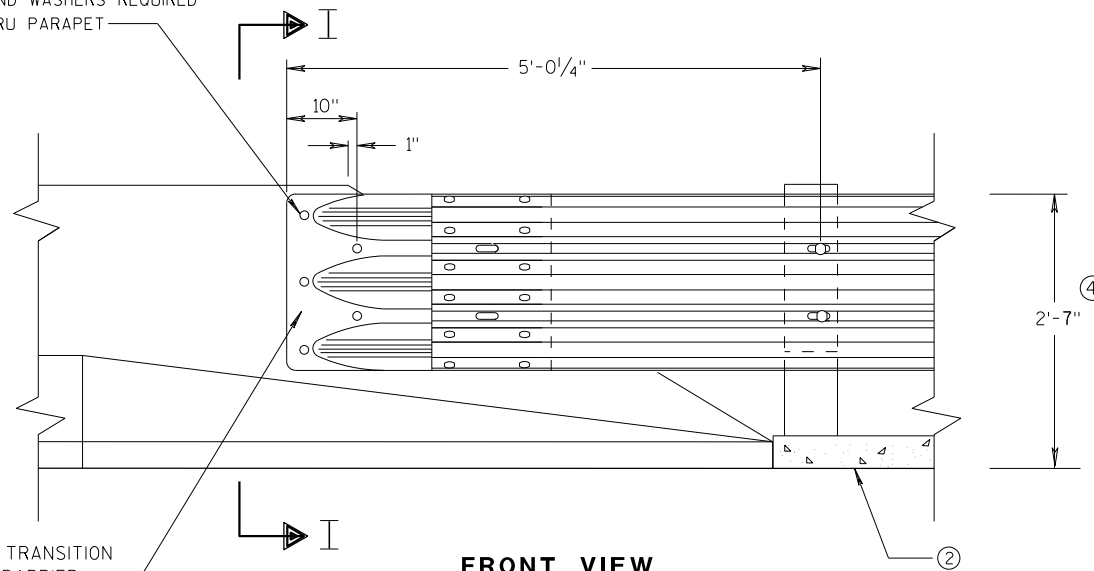
- ② OPTIONAL CURB AND GUTTER OR DRAINAGE FEATURE SEE PLAN FOR INFORMATION.
- ④ TOLERANCE FOR TOP OF BEAM IS $\pm 1"$.
- ⑥ DRILLING BOLT HOLES THROUGH THE PARAPET, BOLTS, NUTS, WASHERS AND REPAIRING DAMAGED CONCRETE ARE INCIDENTAL TO THE CONTRACT.
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**DRILL HOLE LOCATION AND PATTERN
FOR THRIE BEAM CONNECTION**

6

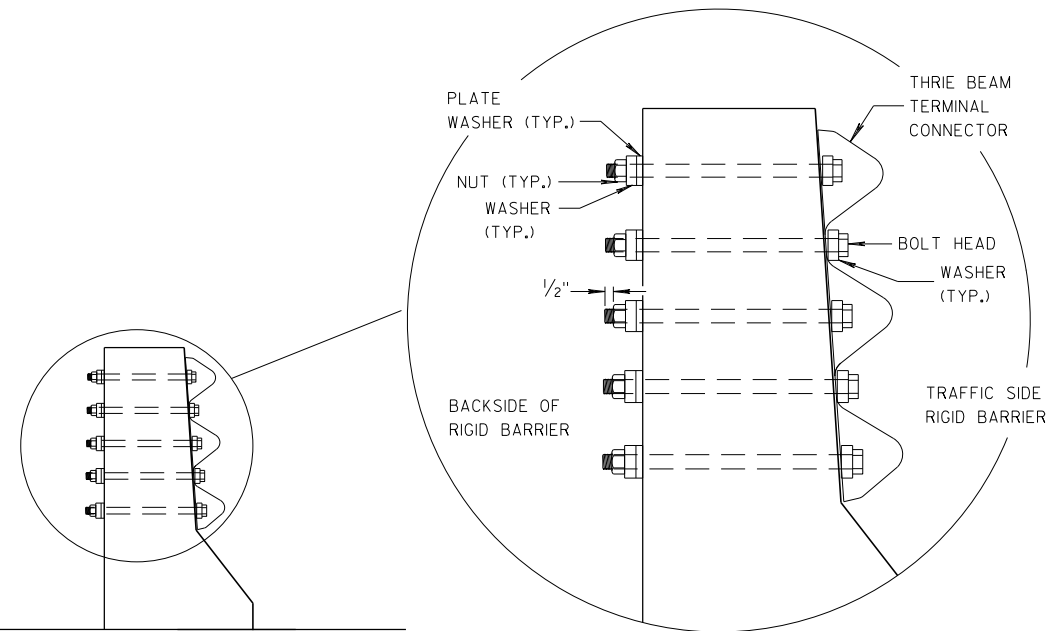
⑥ ⑦ 7/8" DIA. HEX HEAD CAP SCREWS INTO
THREADED INSERTS (FURNISHED WITH THE BRIDGE)
WHEN RETROFITTING INTO AN EXISTING RIGID BARRIER
7/8" DIA. H.S. HEX BOLT AND WASHERS REQUIRED
1" DIA. HOLES DRILLED THRU PARAPET
(5 REQ'D.)



FRONT VIEW

**THRIE BEAM CONNECTION TO BRIDGE
PARAPETS WITH SLOPED ENDS**

WHEN RETROFITTING A TRANSITION
TO AN EXISTING RIGID BARRIER,
INSTALL PLATE WASHERS ON
BACKSIDE OF RIGID BARRIER.



SECTION I-I

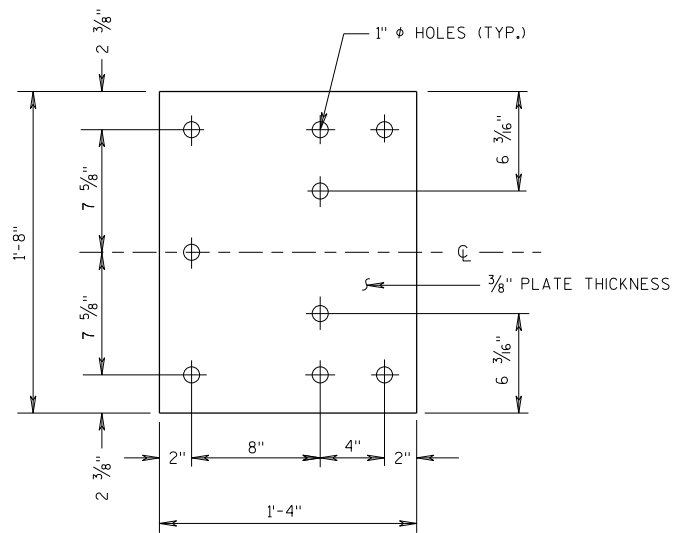
**MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

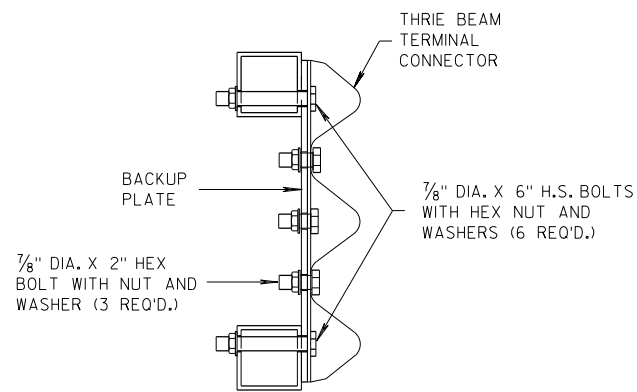
APPROVED
DATE 07/2018 /S/ Rodney Taylor
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UNIT SUPERVISOR
FHWA

S.D.D. 14 B 45-5f

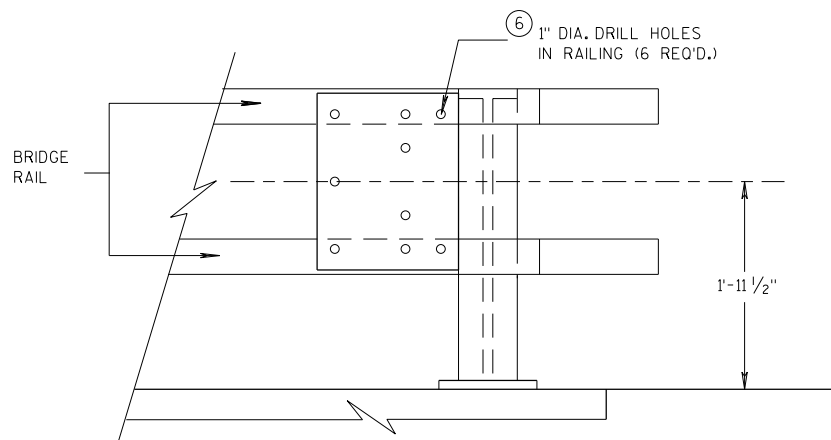
S.D.D. 14 B 45-5f



BACK-UP PLATE DETAIL



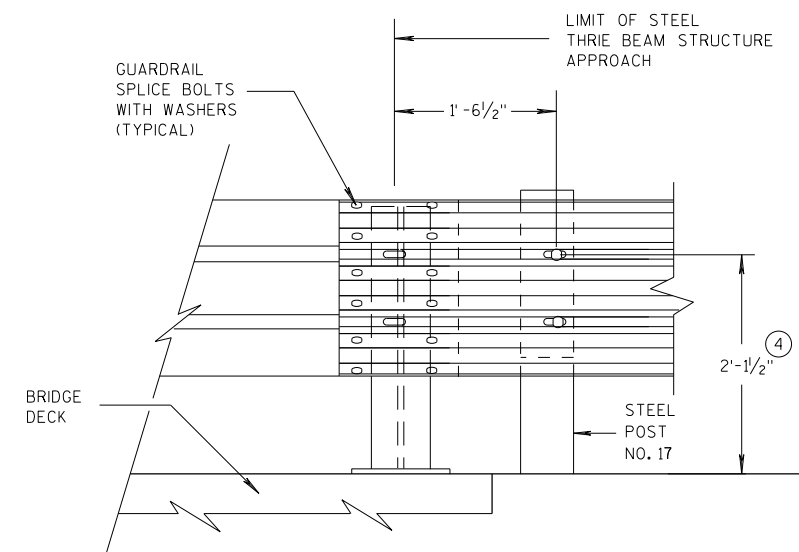
SECTION J-J



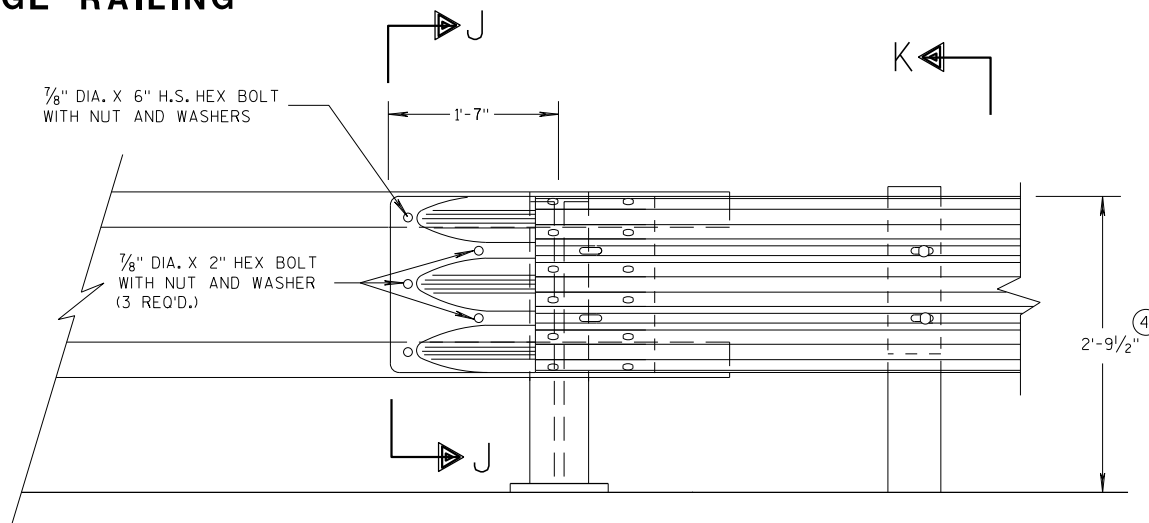
BACK-UP PLATE MOUNTING ONTO BRIDGE RAILING

GENERAL NOTES

- ④ TOLERANCE FOR TOP OF BEAM IS $\pm 1'$.
- ⑥ DRILLING HOLES THROUGH THE PAPER, BOLTS, NUTS, WASHERS AND REPAIRING DAMAGED CONCRETE ARE INCIDENTAL TO THE CONTRACT.

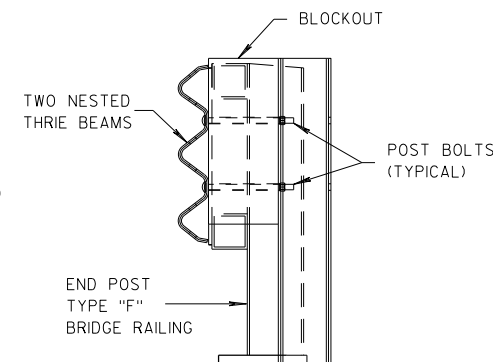


FRONT VIEW THRIE BEAM CONNECTION TO STEEL RAILING TYPE "W"



FRONT VIEW

THRIE BEAM CONNECTION TO TUBULAR RAILING TYPE "F"



SECTION K-K

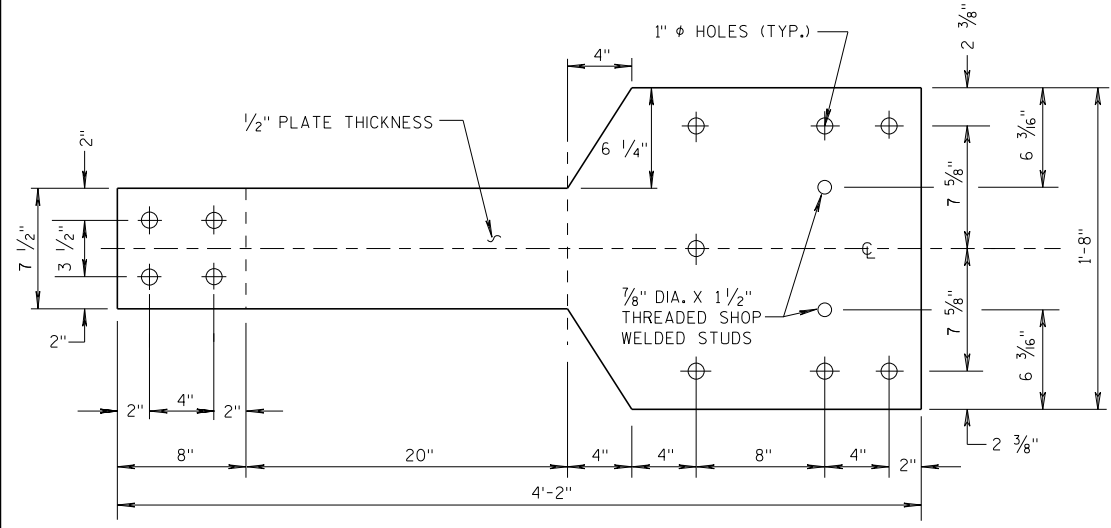
MIDWEST GUARDRAIL SYSTEM THRIE BEAM TRANSITION (MGS)	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 07/2018 DATE	/S/ Rodney Taylor ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR
FHWA	

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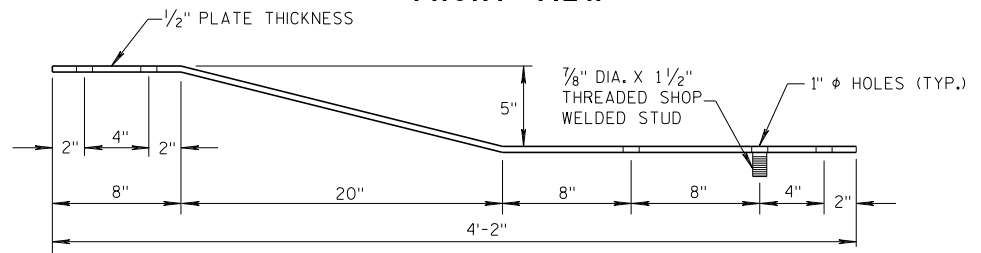
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GENERAL NOTES

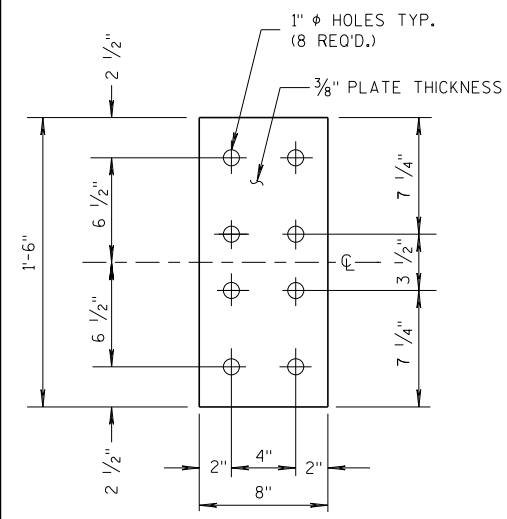
(4) TOLERANCE FOR TOP OF W-BEAM RAIL IS ± 1".



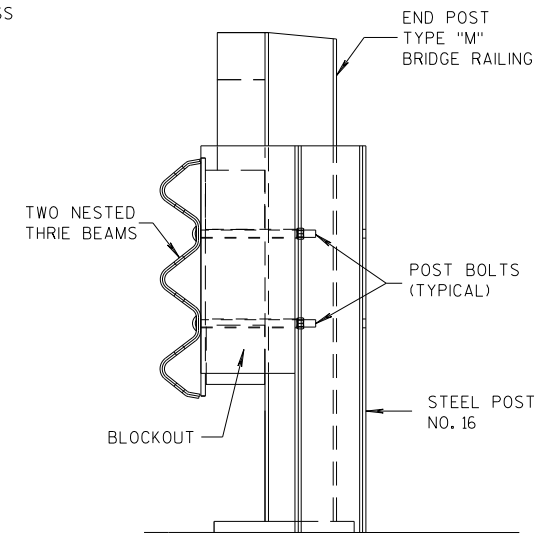
FRONT VIEW



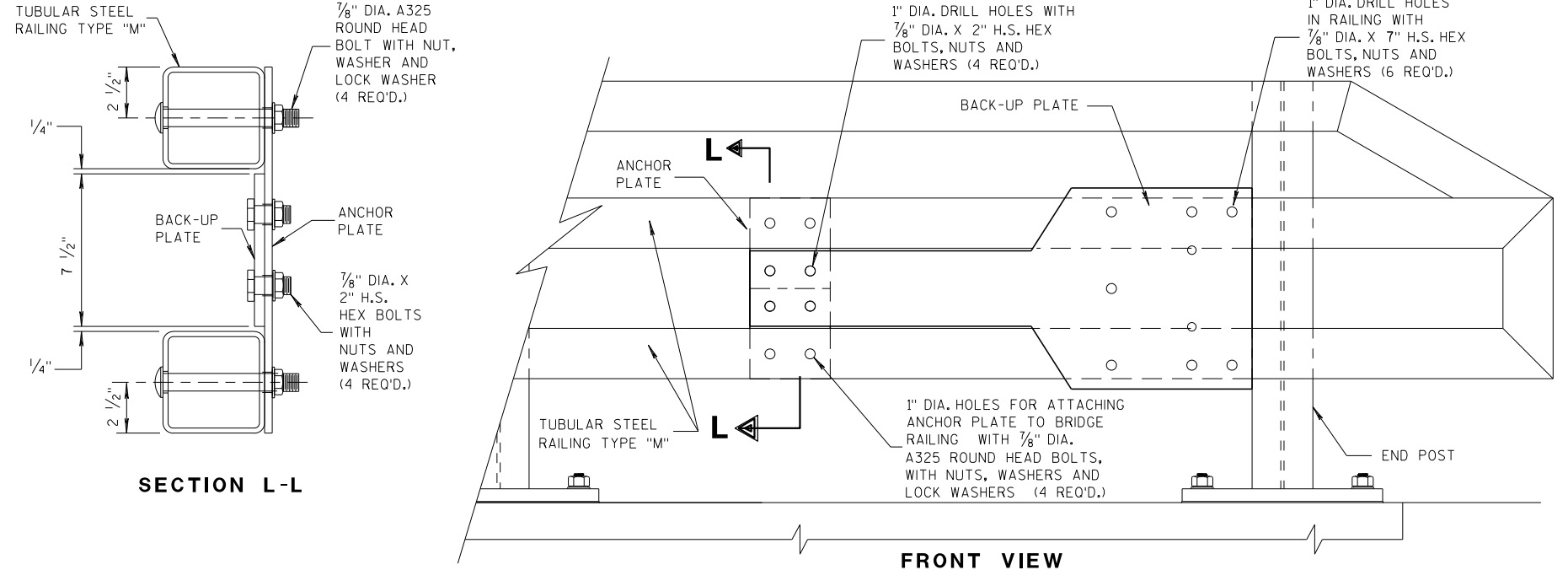
**PLAN VIEW
BACK-UP PLATE DETAIL, TYPE "M"**



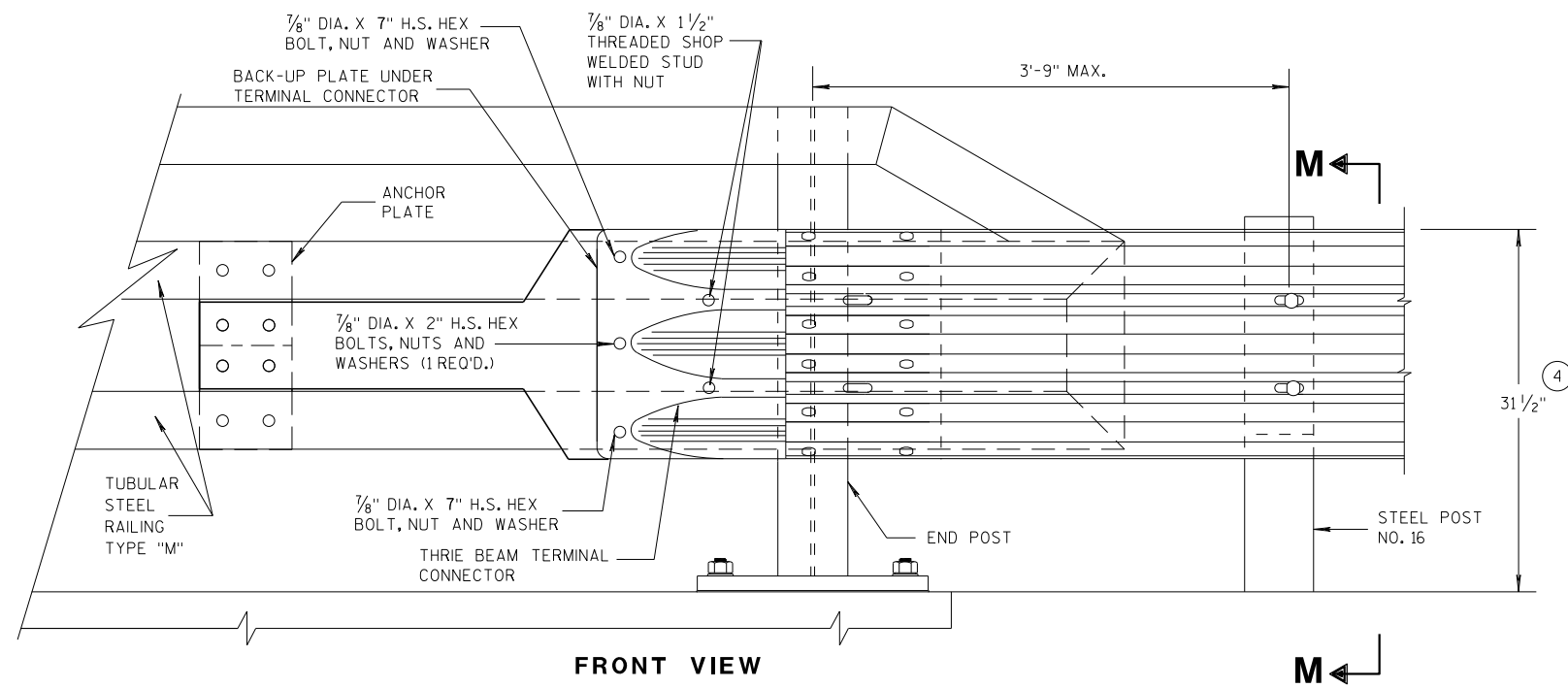
**FRONT VIEW
ANCHOR PLATE DETAIL, TYPE "M"**



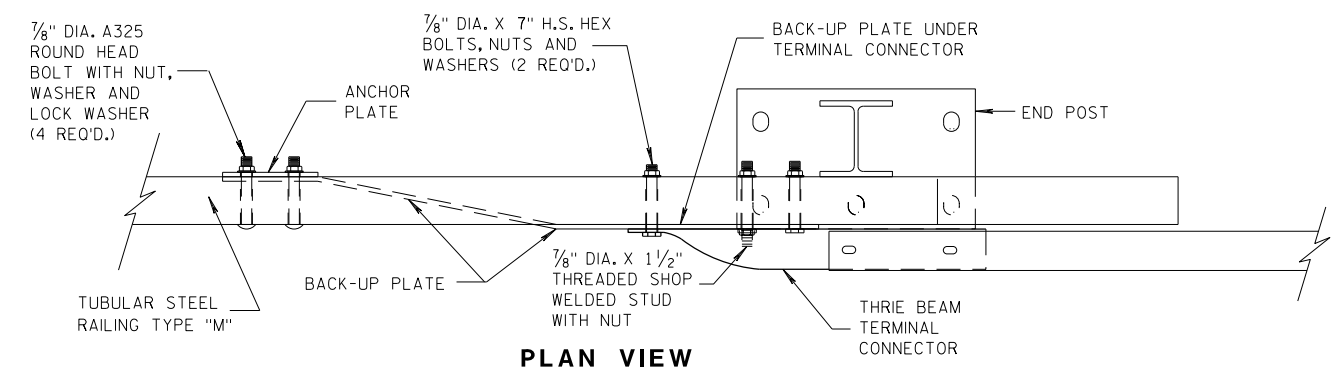
SECTION M-M



ANCHOR AND BACK-UP PLATE MOUNTING TO BRIDGE RAILING, TYPE "M"



FRONT VIEW



PLAN VIEW

THRIE BEAM CONNECTION TO TUBULAR RAILING, TYPE "M"

**MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)**

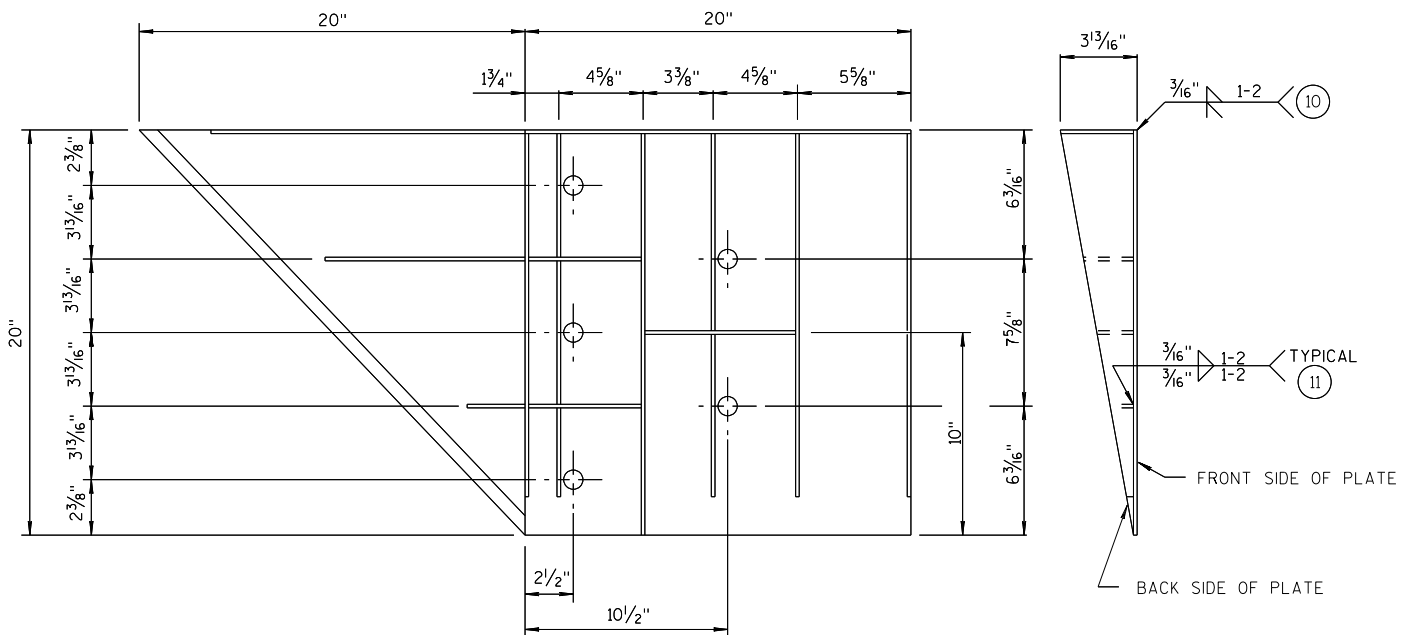
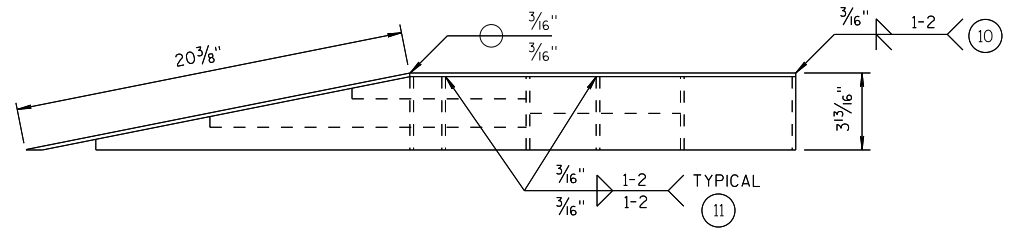
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE 07/2018 /S/ Rodney Taylor
ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR
FHWA

GENERAL NOTES

- COVER PLATE PANELS ARE 3/16" THICK.
- ALL STIFFENERS ARE 1/4" THICK.
- CONNECTOR PLATE SHALL BE FABRICATED FROM ASTM GRADE A36 STEEL AND GALVANIZED.
- FOR GALVANIZED REQUIREMENTS, SEE SECTION 614 OF THE STANDARD SPECIFICATIONS.
- ALL HOLE DIAMETERS SHALL BE 1".
- FOR OPPOSITE SIDE INSTALLATION MIRROR DRAWINGS.

- (10) STIFFENERS LOCATED AT THE OUTSIDE EDGES OF THE COVER PLATES SHALL BE WELDED AS FOLLOWS:
SINGLE BEVEL GROOVE WELD ON EXTERNAL SIDES AND 3/16" FILLET WELD BY 1" LONG SPACED AT 2" ON INTERNAL SIDES.
- (11) STIFFENERS LOCATED ON THE INSIDE OF THE COVER PLATE SHALL BE WELDED AS FOLLOWS:
3/16" FILLET WELD BY 1" LONG SPACED AT 2".



WELDING INSTRUCTION
(VIEWED FROM BACK SIDE OF PLATE)

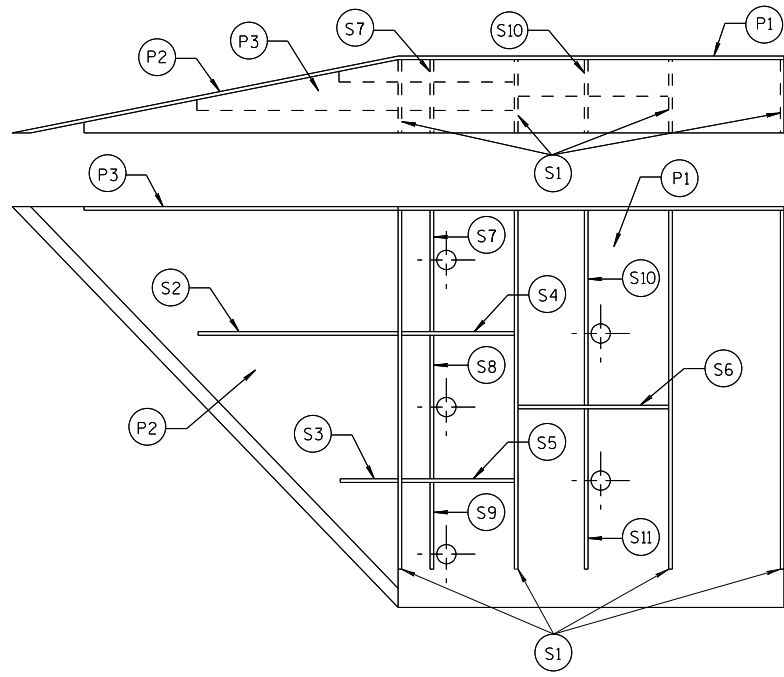


PLATE AND STIFFENER IDENTIFICATION
(VIEWED FROM BACK SIDE OF PLATE)

CONNECTOR PLATE DIMENSION (PER ASSEMBLY)				
PLATE	QUANTITY	SHAPE	SIZE (A x B x C x D)	THICKNESS
P1	1		20" x 20"	3/16"
P2	1		20" x 20" x 28 3/16"	3/16"
P3	1		39" x 3 5/8" x 20" x 19 5/16"	3/16"
S1	4		18 7/16" x 3 5/8" x 18 3/4"	1/4"
S2	1		10 1/4" x 2 1/16" x 10 3/8" x 1/2"	1/4"
S3	1		3" x 1 1/16" x 3 3/8" x 1/2"	1/4"
S4	1		6 1/8" x 2 1/16"	1/4"
S5	1		6 1/8" x 1 1/16"	1/4"
S6	1		7 3/4" x 1 3/4"	1/4"
S7	1		2 3/16" x 6" x 3 5/8" x 5 1/8"	1/4"
S8	1		1 5/32" x 7 1/2" x 2 1/2" x 7 3/8"	1/4"
S9	1		6 1/16" x 6 3/16" x 1 3/32"	1/4"
S10	1		1 7/8" x 9 7/8" x 3 3/8" x 9 11/16"	1/4"
S11	1		8 1/2" x 8 3/4" x 1 3/16"	1/4"

SINGLE SLOPE CONNECTION PLATE

**MIDWEST GUARDRAIL SYSTEM
THREE BEAM TRANSITION (MGS)**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED: _____ /S/ Rodney Taylor
DATE: 7/2018 ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR
FHWA

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S.D.D. 14 B 45-51

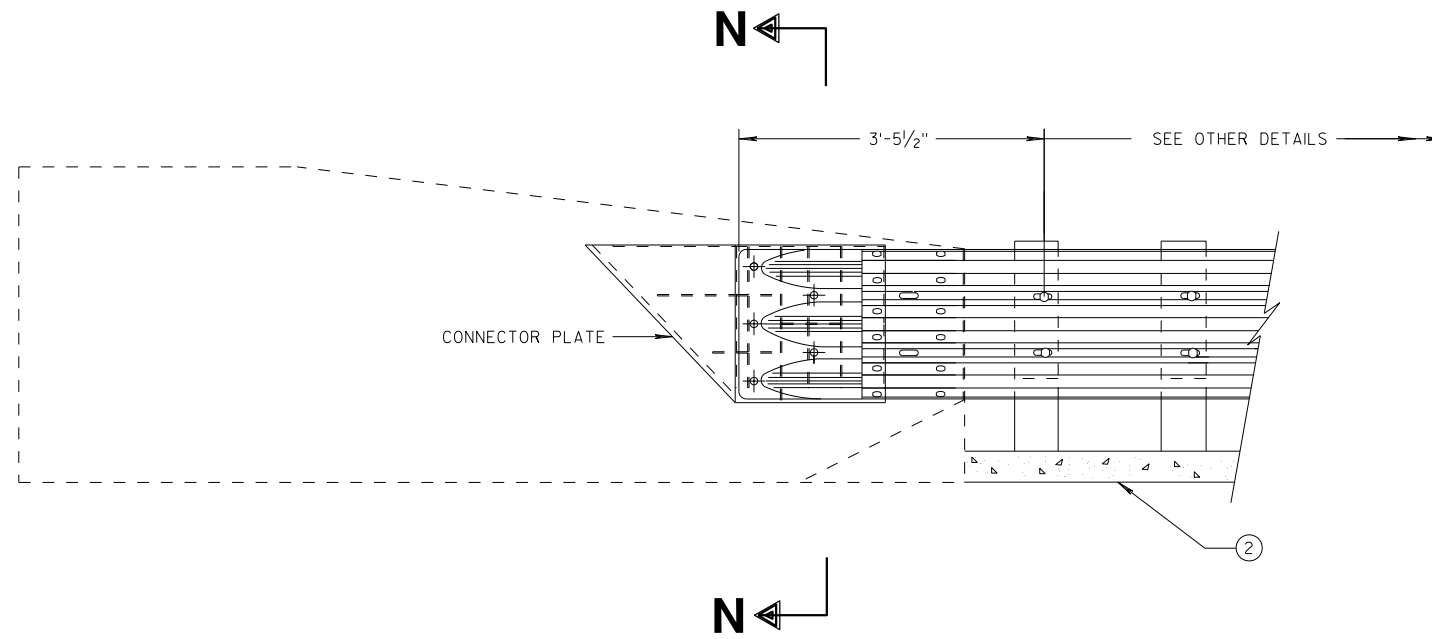
S.D.D. 14 B 45-51

GENERAL NOTES

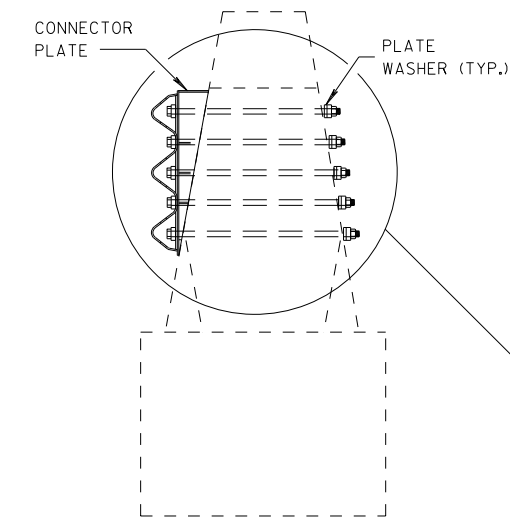
CONNECTOR PLATE, DRILLING BOLT HOLES THROUGH THE PARAPET, BOLTS, NUTS, WASHERS AND REPAIRING DAMAGED CONCRETE ARE INCIDENTAL TO THE CONTRACT.

② OPTIONAL CURB AND GUTTER OR DRAINAGE FEATURE SEE PLAN FOR INFORMATION.

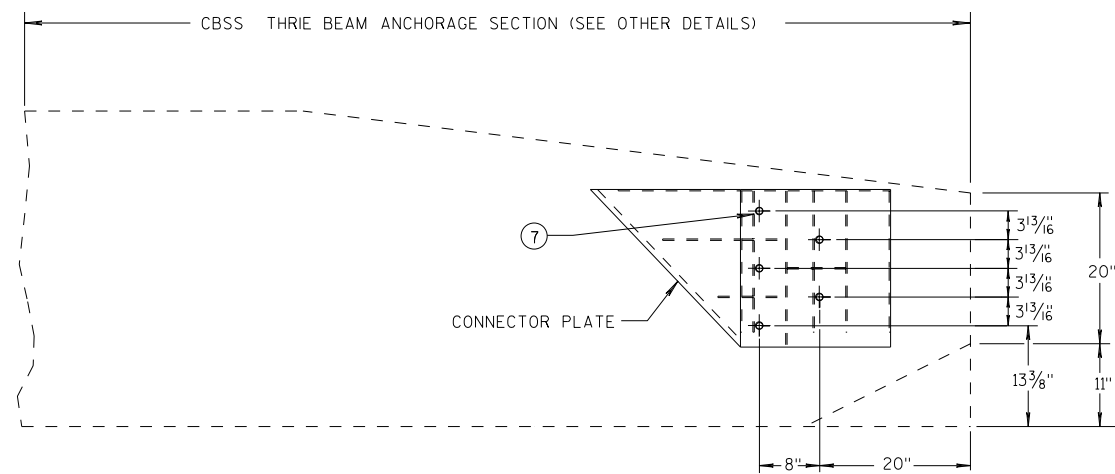
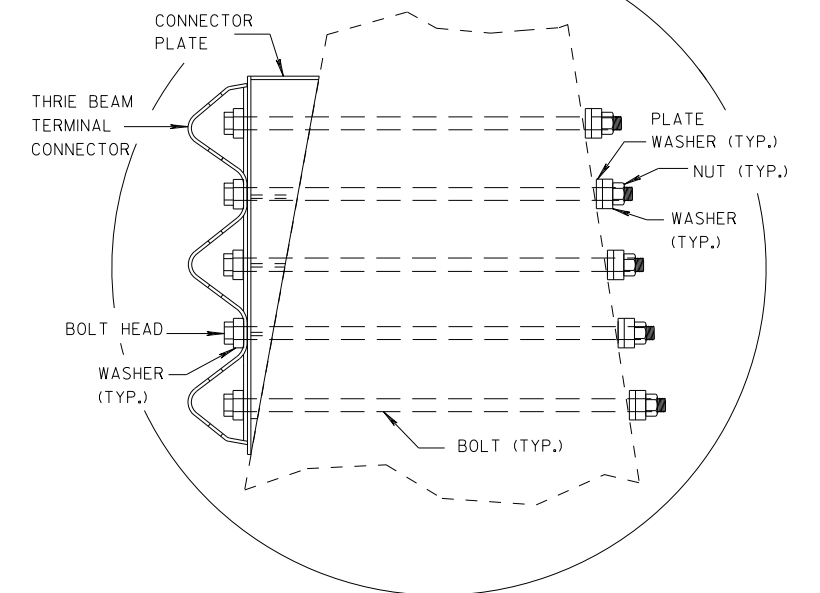
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THRIE BEAM CONNECTION TO SINGLE SLOPE BARRIER



SECTION N-N

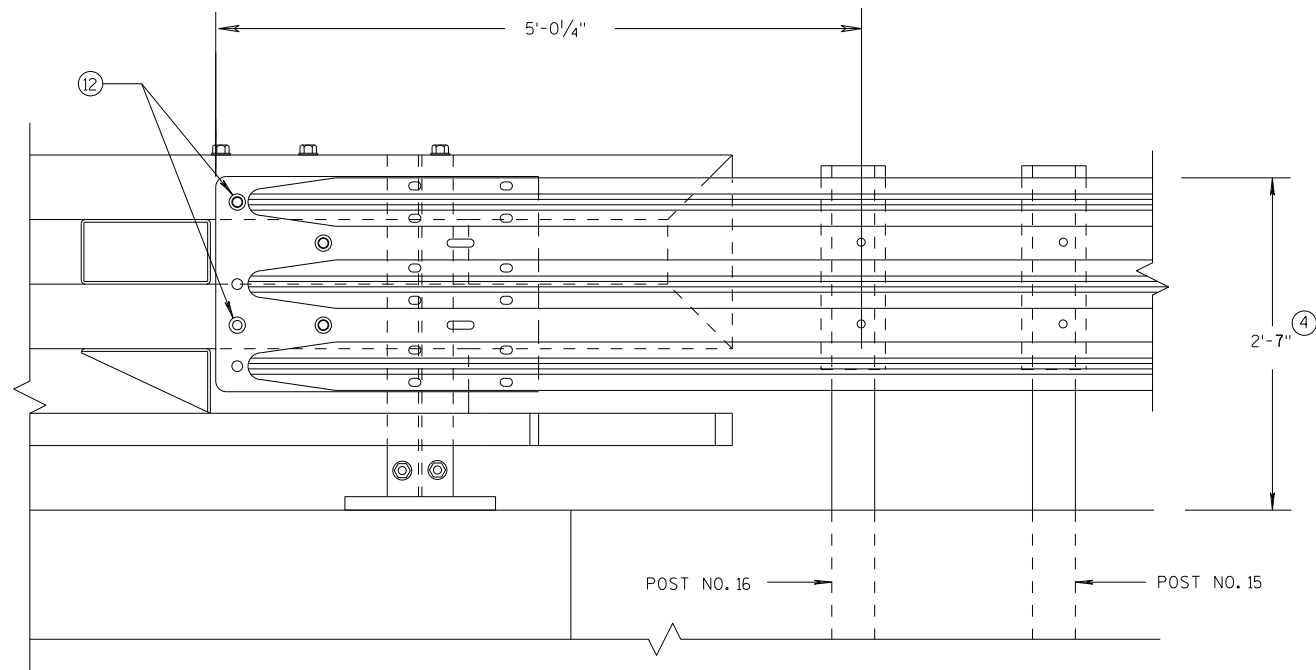


SINGLE SLOPE CONNECTION PLATE PLACEMENT

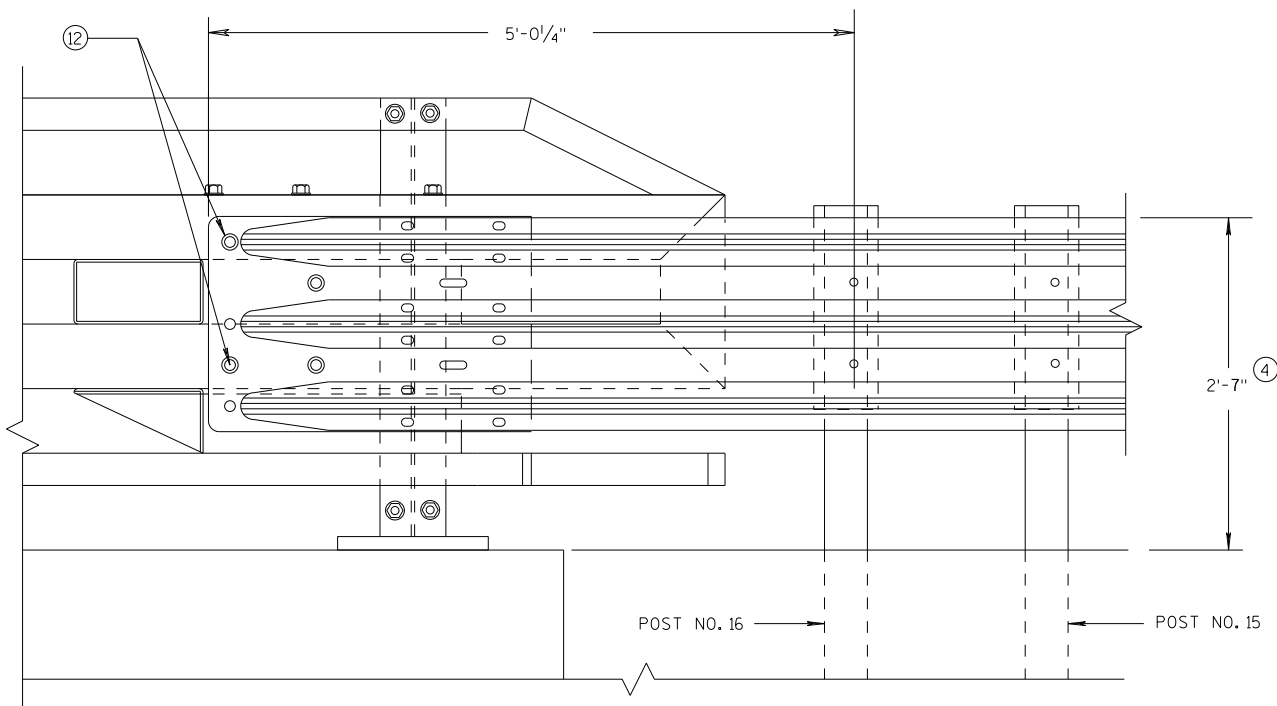
**MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE 7/2018 /S/ Rodney Taylor
ROADWAY STANDARDS DEVELOPMENT
UNIT SUPERVISOR
FHWA



**ELEVATION OF DETAIL AT NY3 END POST
THRIE BEAM RAIL ATTACHMENT**



**ELEVATION OF DETAIL AT NY4 END POST
THRIE BEAM RAIL ATTACHMENT**

GENERAL NOTES

- ④ TOLERANCE FOR TOP OF BEAM IS ± 1".
- ⑫ BOLTS MAY BE A325 BOLTS OR A449 BOLTS. BOLT LENGTH AND THREADING LENGTH ARE TO ALLOW FOR A TIGHT CONNECTION BETWEEN RIGID BARRIER AND THRIE BEAM CONNECTION PLATE. CONTRACTOR IS TO FIELD VERIFY BOLT LENGTH AND THREAD LENGTH. ONE ROUND WASHER REQUIRED BETWEEN BOLT HEAD AND THRIE BEAM CONNECTOR PLATE. ON BACKSIDE OF PARAPET ONE ROUND WASHER, AND NUT REQUIRED. BOLT THREAD IS TO EXTEND 1/2-INCH BEYOND NUT.

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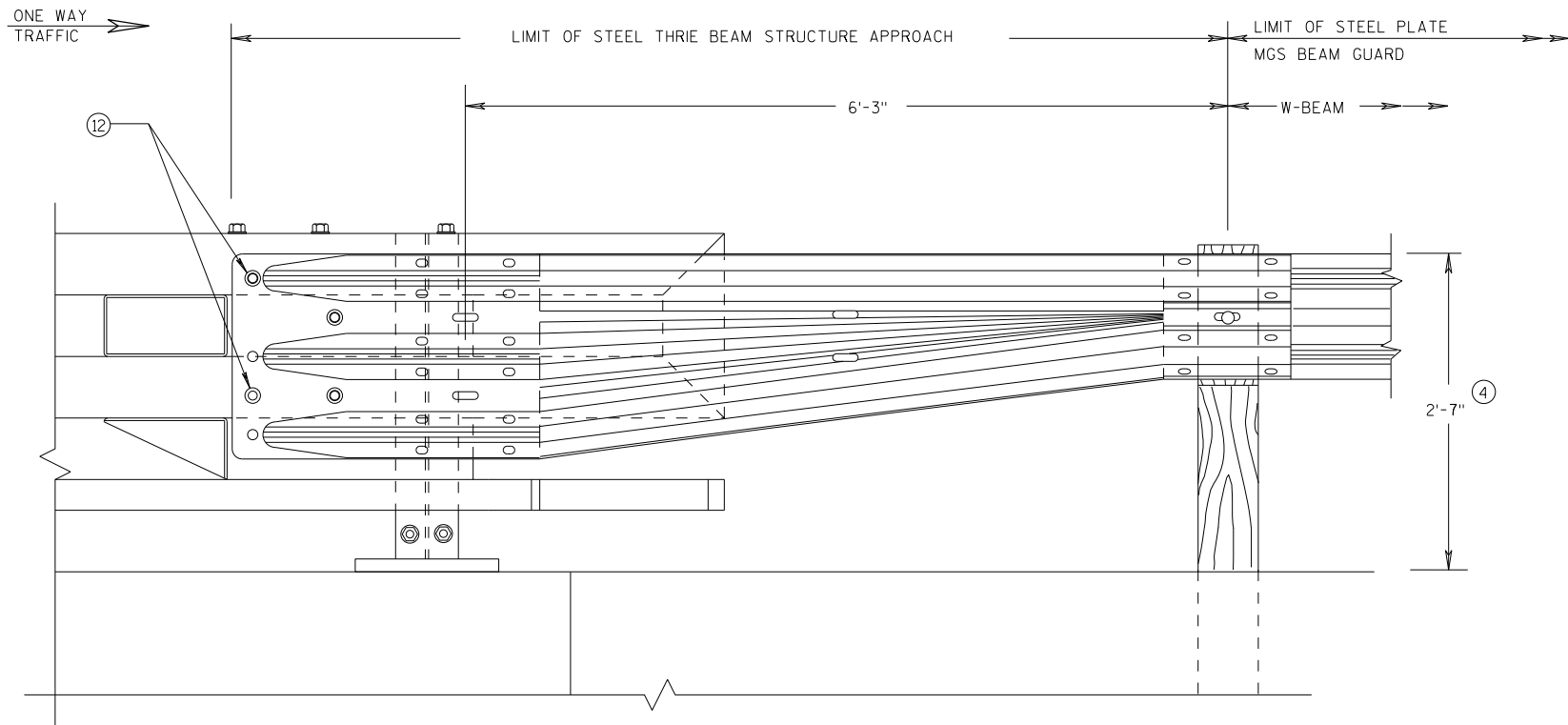
S.D.D. 14 B 45-5k

S.D.D. 14 B 45-5k

**MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

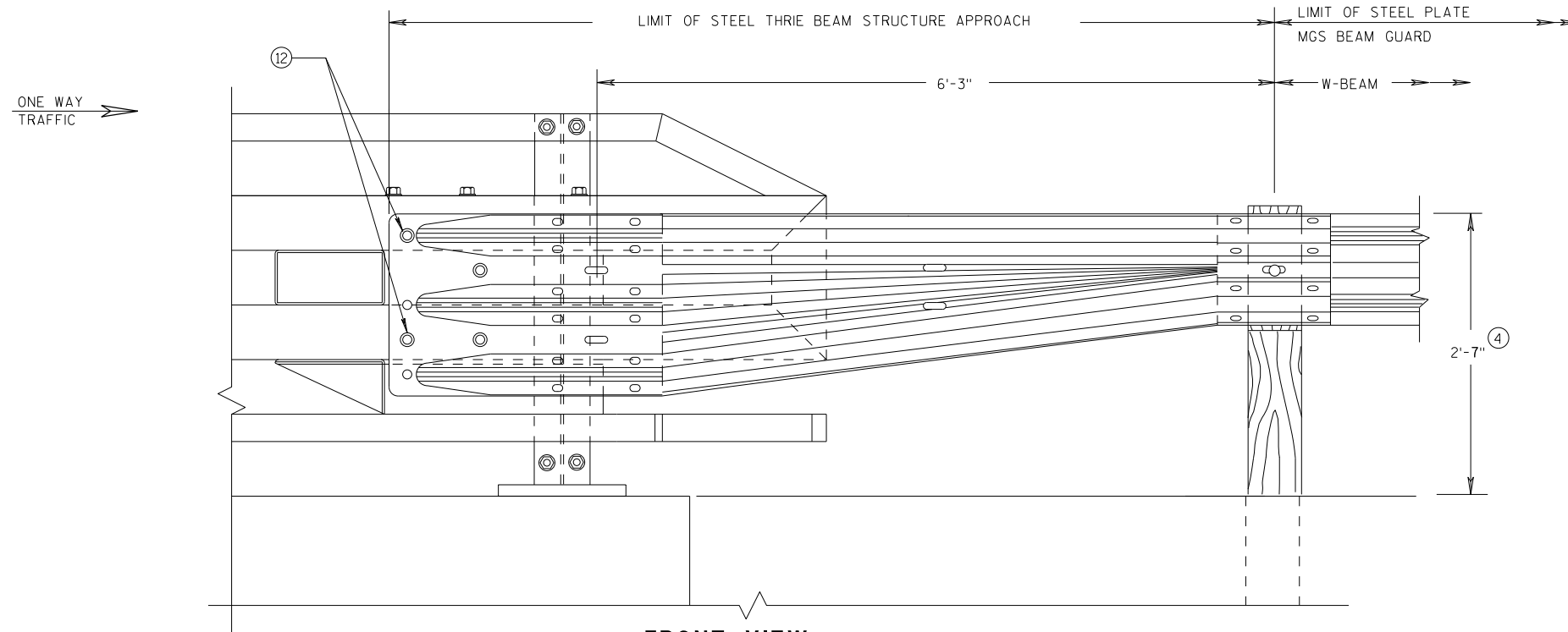
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DATE 7/2018 /S/ Rodney Taylor
ROADWAY STANDARDS DEVELOPMENT
UNIT SUPERVISOR
FHWA



FRONT VIEW
W BEAM TRANSITION AND
CONNECTION TO BRIDGE RAILING TYPE "NY3"
 (USE ONLY ON THE TRAFFIC EXIT END OF ONE WAY BRIDGES)

GENERAL NOTES

- ④ TOLERANCE FOR TOP OF BEAM IS $\pm 1"$.
- ⑫ BOLTS MAY BE A325 BOLTS OR A449 BOLTS. BOLT LENGTH AND THREADING LENGTH ARE TO ALLOW FOR A TIGHT CONNECTION BETWEEN RIGID BARRIER AND THRIE BEAM CONNECTION PLATE. CONTRACTOR IS TO FIELD VERIFY BOLT LENGTH AND THREAD LENGTH. ONE ROUND WASHER REQUIRED BETWEEN BOLT HEAD AND THRIE BEAM CONNECTOR PLATE. ON BACKSIDE OF PARAPET ONE ROUND WASHER, AND NUT REQUIRED. BOLT THREAD IS TO EXTEND $\frac{1}{2}$ -INCH BEYOND NUT.

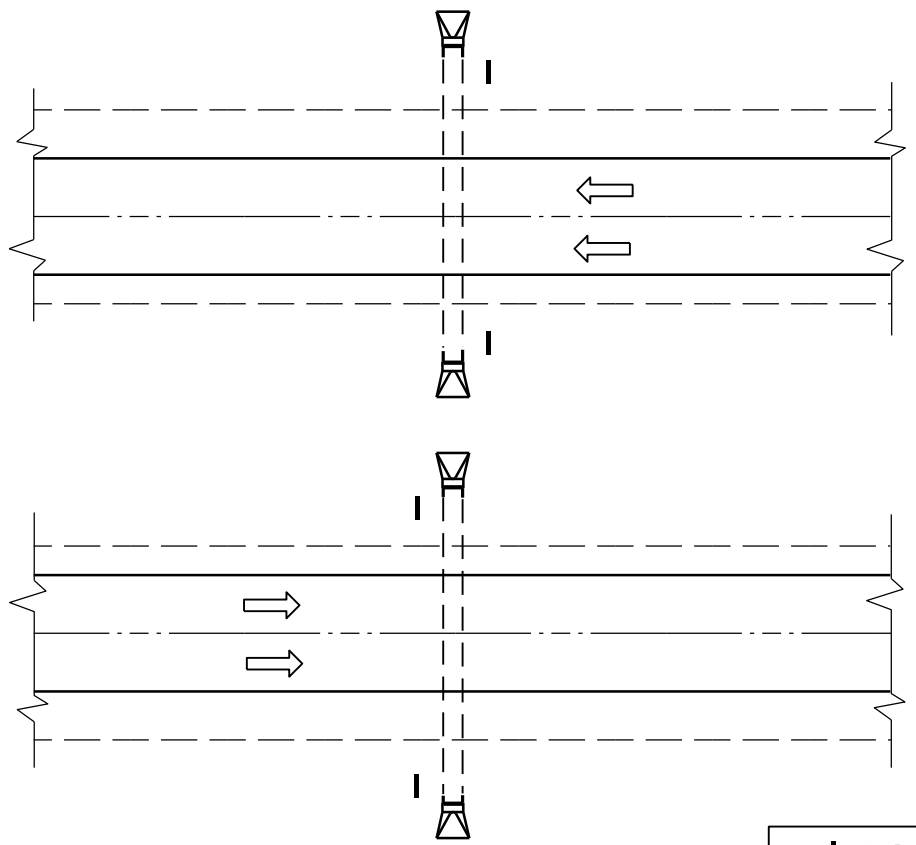


FRONT VIEW
W BEAM TRANSITION AND
CONNECTION TO BRIDGE RAILING TYPE "NY4"
 (USE ONLY ON THE TRAFFIC EXIT END OF ONE WAY BRIDGES)

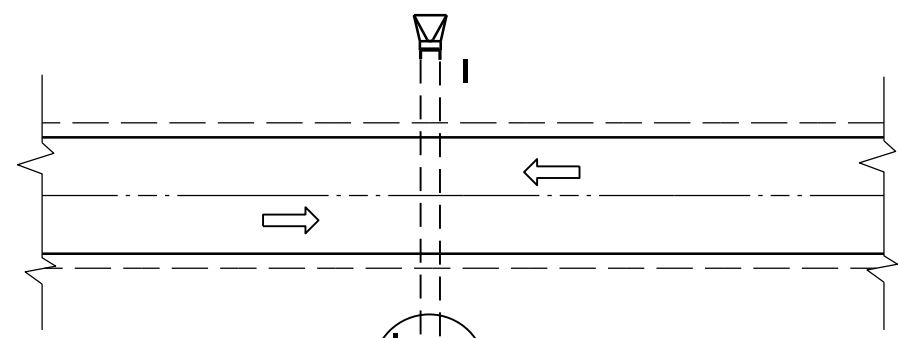
MIDWEST GUARDRAIL SYSTEM
THRIE BEAM TRANSITION (MGS)

STATE OF WISCONSIN
 DEPARTMENT OF TRANSPORTATION

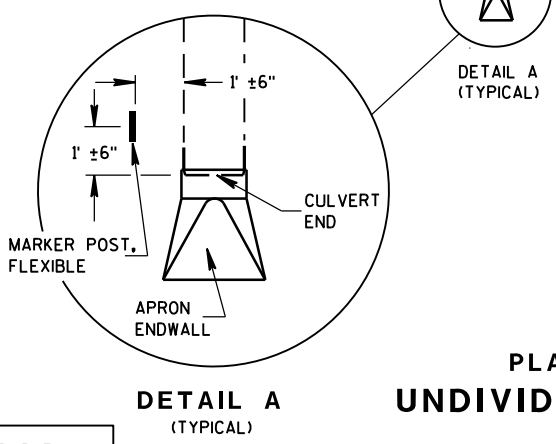
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 DATE 7/2018 /S/ Rodney Taylor
 ROADWAY STANDARDS DEVELOPMENT
 UNIT SUPERVISOR
 FHWA



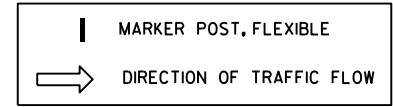
PLAN VIEW
DIVIDED HIGHWAY



PLAN VIEW
UNDIVIDED HIGHWAY

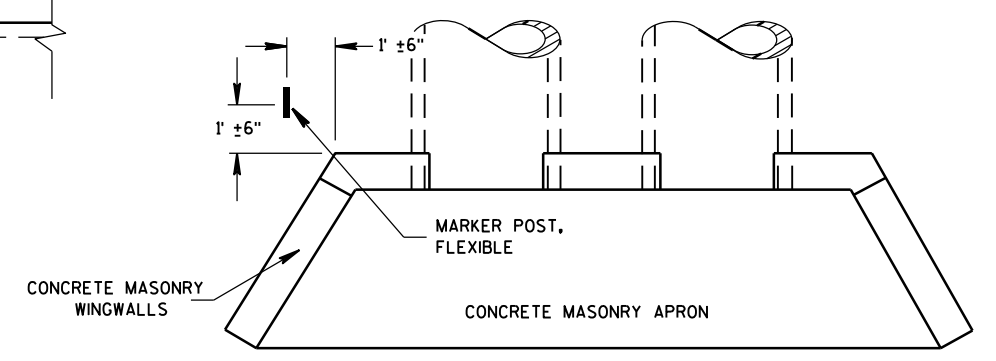


DETAIL A
(TYPICAL)



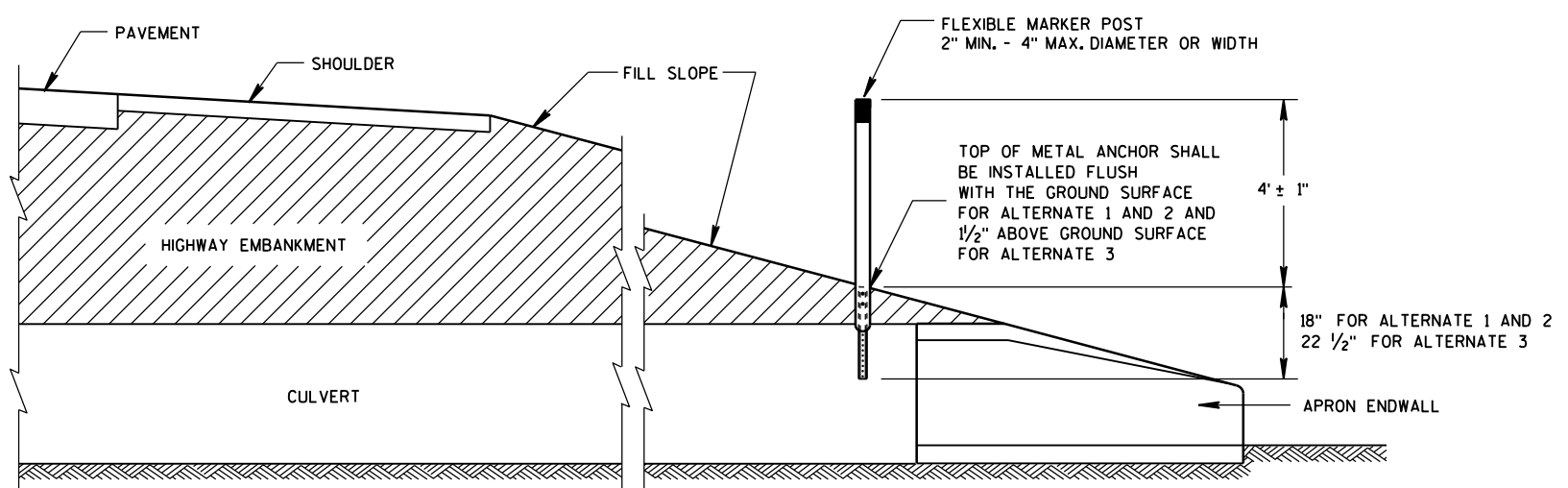
GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.



PLAN VIEW
CONCRETE MASONRY ENDWALLS FOR
CULVERT PIPE AND PIPE ARCH

FLEXIBLE MARKER POST LOCATION



CROSS SECTION
FLEXIBLE MARKER POST

**FLEXIBLE MARKER POST
FOR CULVERT END**

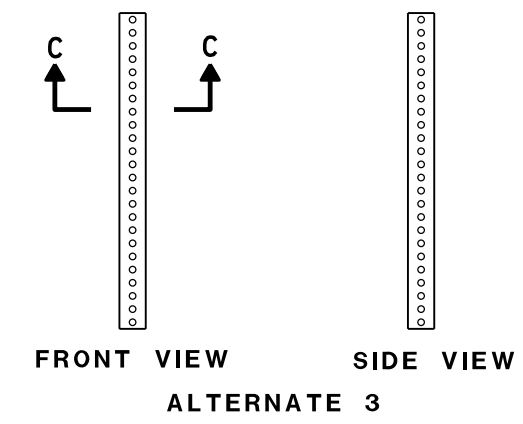
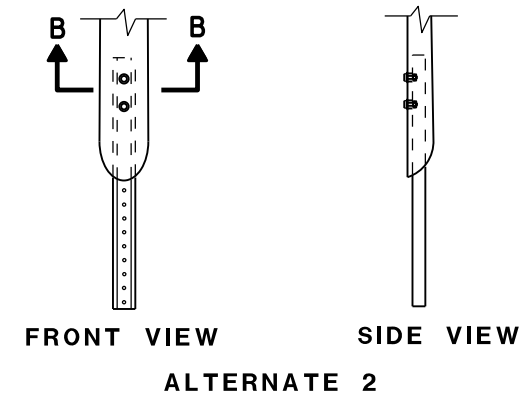
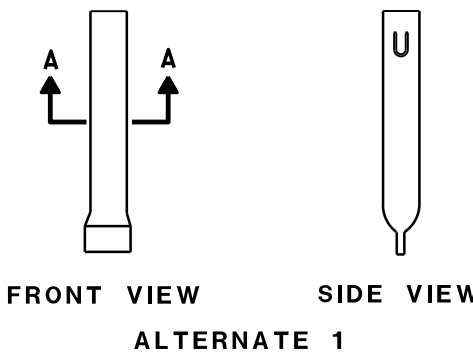
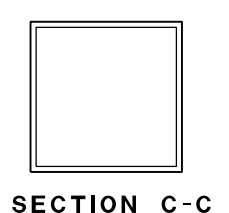
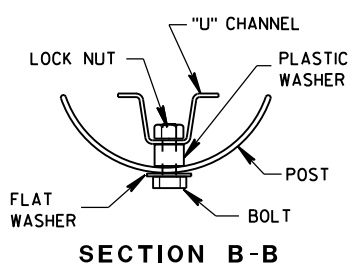
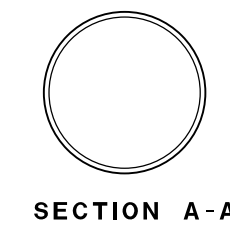
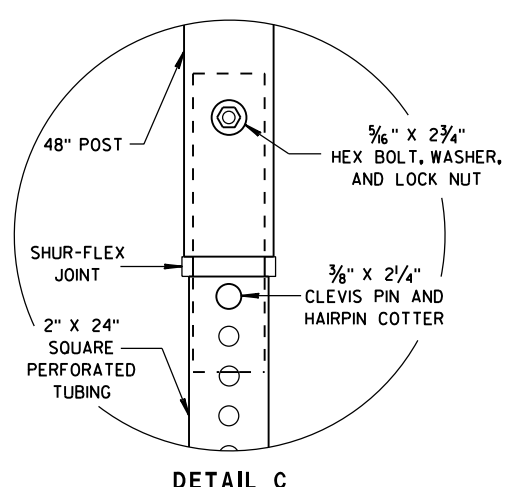
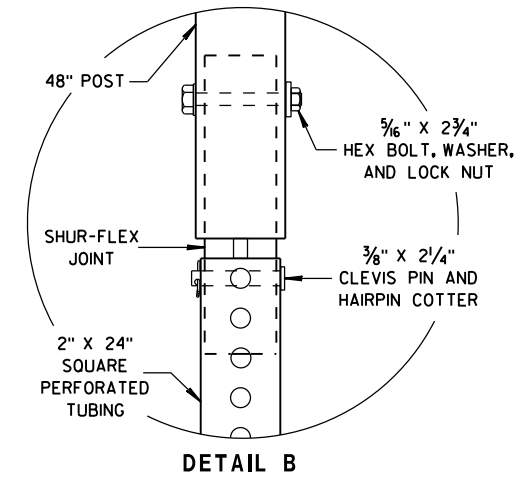
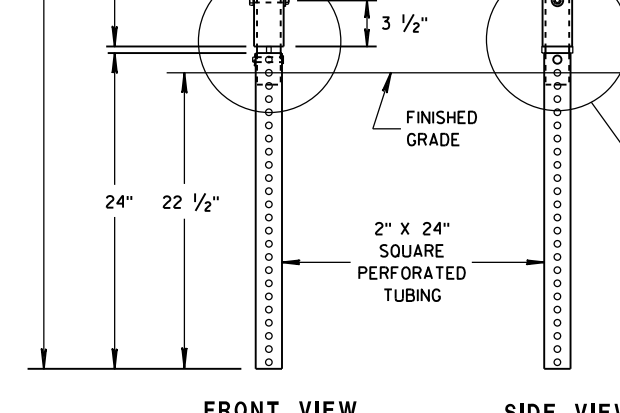
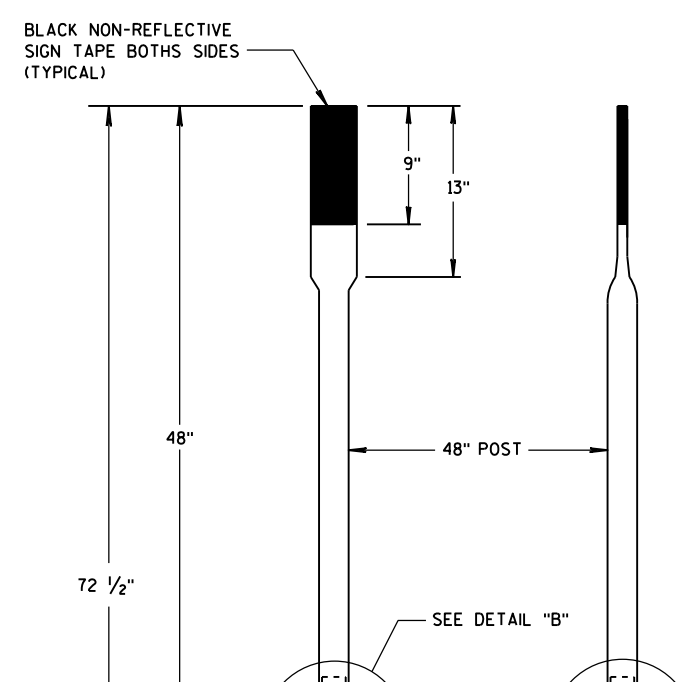
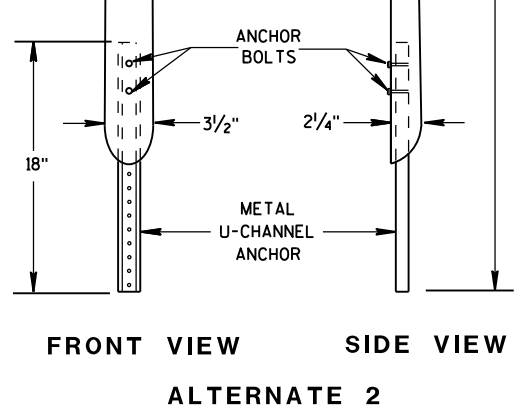
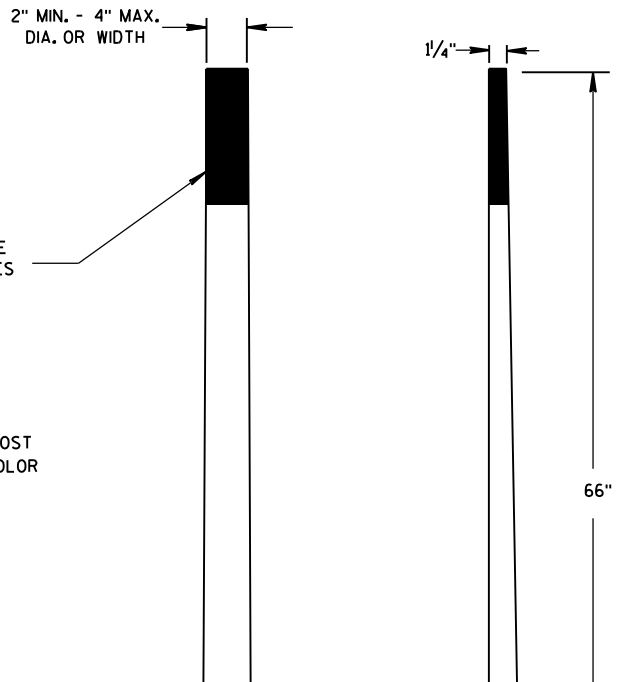
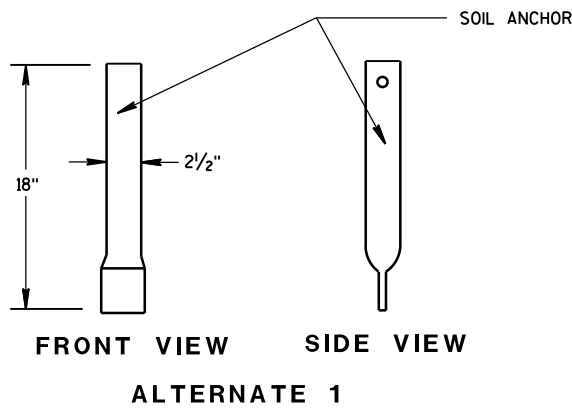
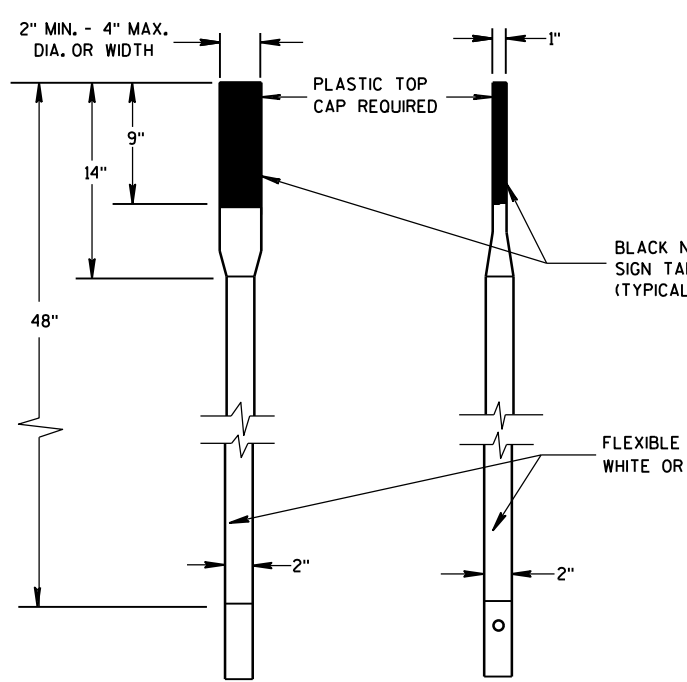
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

6

S.D.D. 15 A 3-2a

S.D.D. 15 A 3-2a



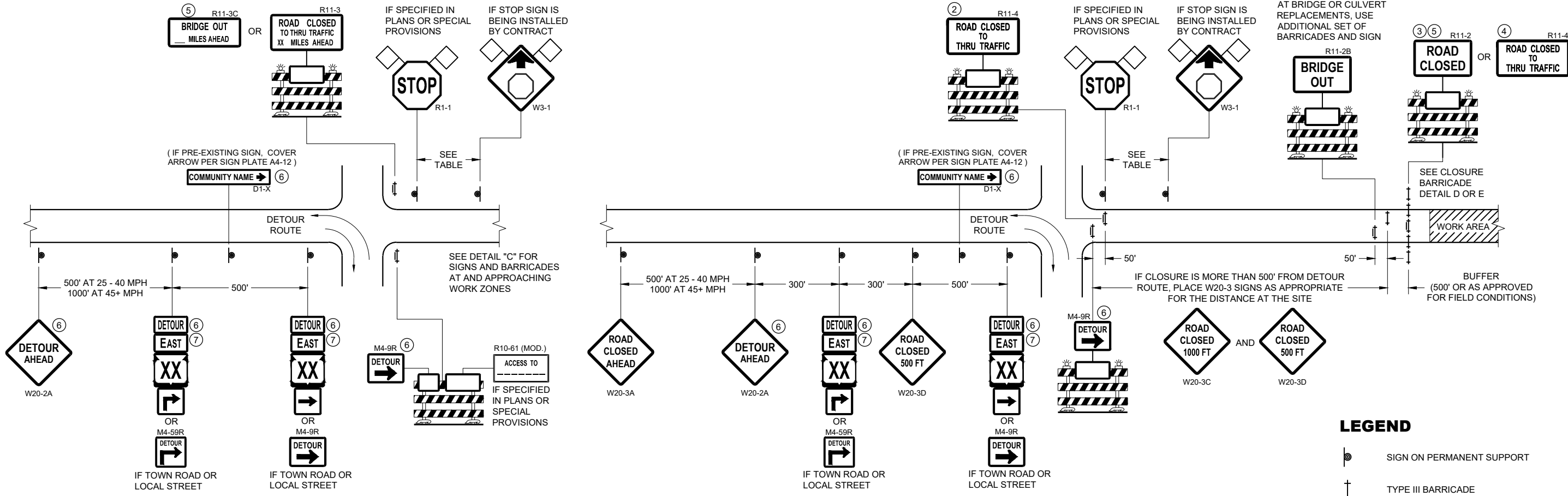
FLEXIBLE MARKER POSTS

FLEXIBLE MARKER POST ANCHORS

FLEXIBLE MARKER POST FOR CULVERT END

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
10/1/2012 DATE /S/ Travis Feltes
STATE TRAFFIC ENGINEER OF DESIGN
FHWA



**DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR**

WORK ZONE GREATER THAN OR EQUAL TO 1/2 MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)

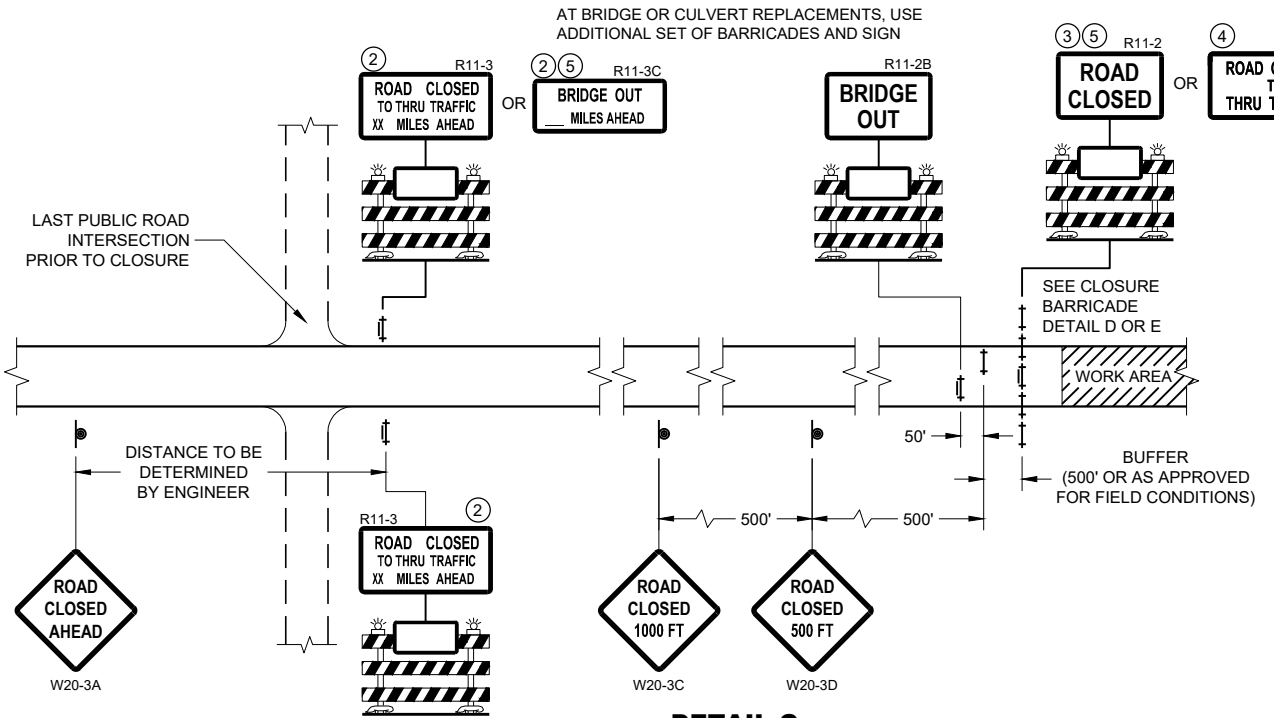
**DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR**

WORK ZONE LESS THAN 1/2 MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)

- LEGEND**
- SIGN ON PERMANENT SUPPORT
 - TYPE III BARRICADE
 - TYPE III BARRICADE WITH ATTACHED SIGN
 - TYPE "A" WARNING LIGHT (FLASHING)
 - WORK AREA
 - FLAGS, 16" X 16" MIN. (ORANGE)

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

- M4 - 8
- M3 - X
- OR OR M1 - 4 M1 - 6 M1 - 5A
- OR M05 - 1 M06 - 1



**DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR**

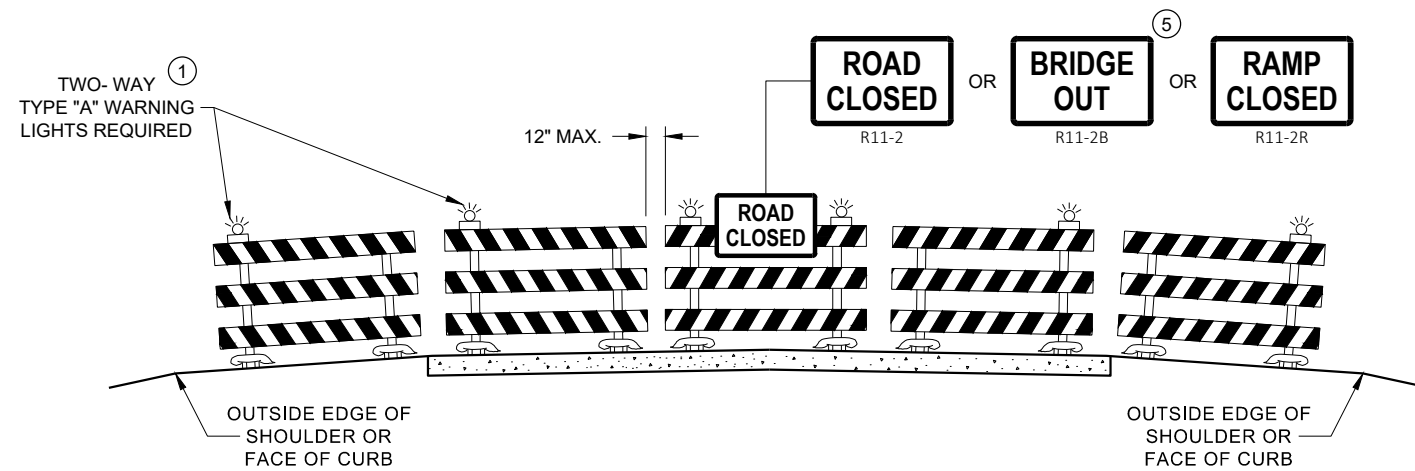
SEE SDD 15C2-SHEET "b"
FOR GENERAL NOTES
AND FOOTNOTES ① THROUGH ⑦

**BARRICADES AND SIGNS
FOR MAINLINE CLOSURES**

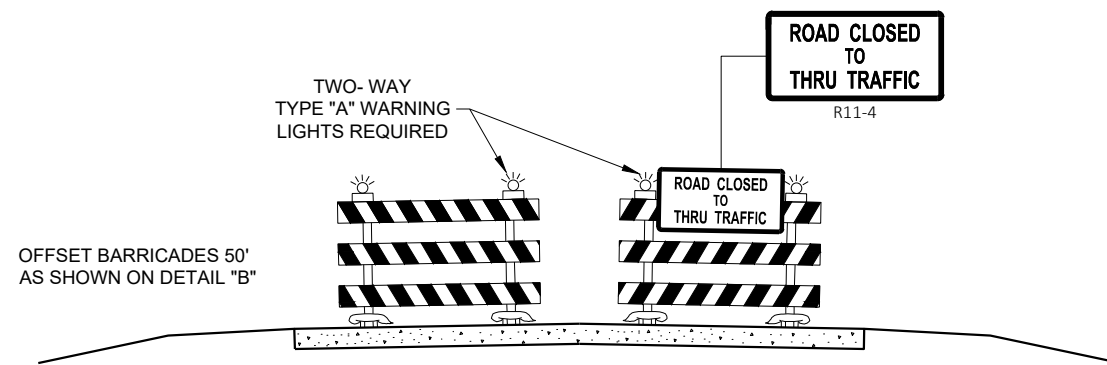
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 /S/ Andrew Heidtke
DATE DATE WORK ZONE ENGINEER

FHWA



**DETAIL D
ROAD CLOSURE BARRICADE DETAIL
APPROACH VIEW**



**DETAIL E
LANE CLOSURE BARRICADE DETAIL
APPROACH VIEW**

SEE SDD 15C2 - SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW - INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11 - 2, R11 - 3, M4 - 9, R11 - 4, AND R10 - 61 SIGNS PLACED ON THE BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE RAIL OR BOTTOM RAILS.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

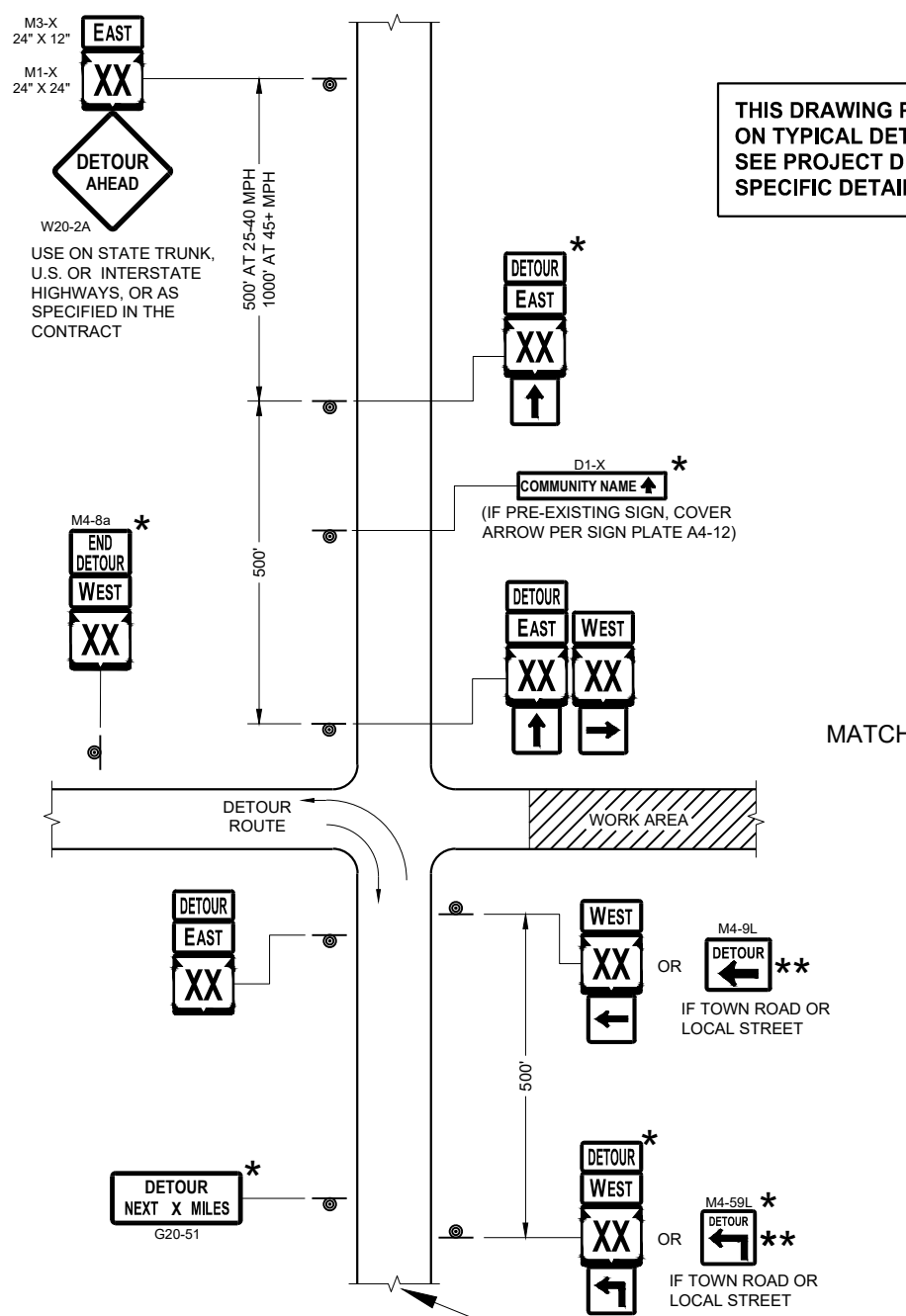
- R11 - 2 SHALL BE 48" X 30"
- R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60" X 30"
- M4 - 9 SHALL BE 30" X 24"
- M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1 - 1 SHALL BE 36" X 36"

- ① TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT SPACING).
- ② THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.
- ③ FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".
- ④ FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".
- ⑤ FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.
- ⑥ INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.
- ⑦ "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

**BARRICADES AND SIGNS
FOR
VARIOUS CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER
FHWA



THIS DRAWING PROVIDES GENERAL GUIDANCE ON TYPICAL DETOUR SIGN LAYOUT AND SPACING. SEE PROJECT DETOUR SIGNING SHEETS FOR SPECIFIC DETAILS FOR EACH PROJECT.

LEGEND

- SIGN ON PERMANENT SUPPORT
- WORK AREA
- M4 - 8
- M3 - X
- M1 - 4
- M1 - 6
- M1 - 5A
- M05 - 1
- M06 - 1
- M06 - 1

GENERAL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. MODIFY EXISTING SIGNS WHERE POSSIBLE.

THE SPACING BETWEEN TRAFFIC CONTROL AND DETOUR SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

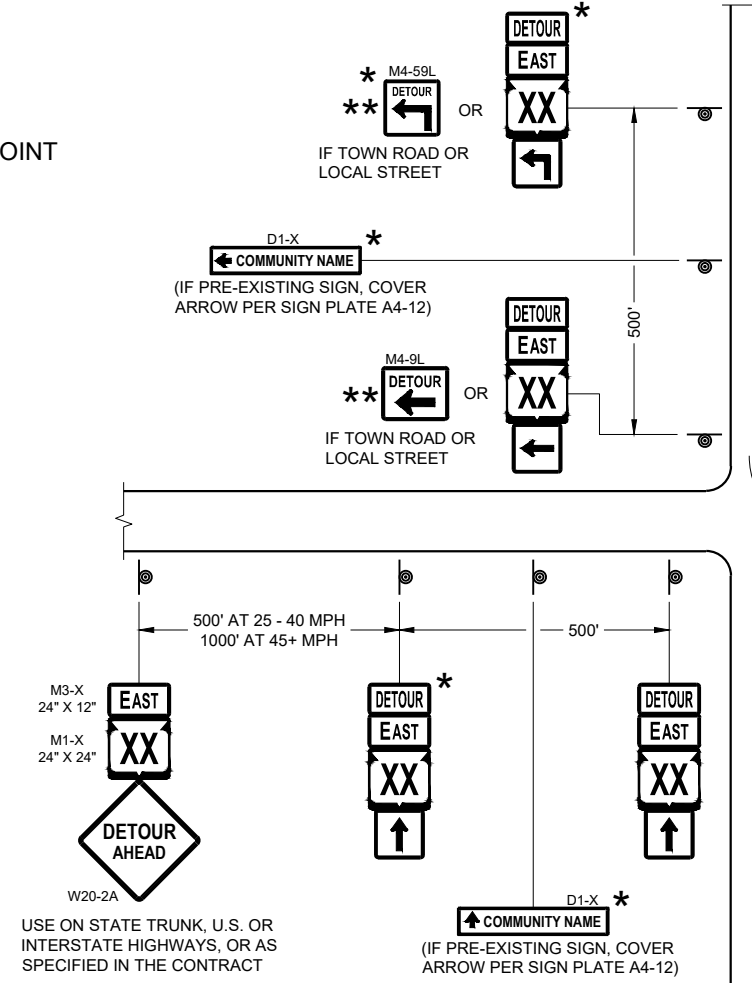
"MO" SIGNS ARE THE SAME AS "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGN SIZES SHALL BE AS FOLLOWS:

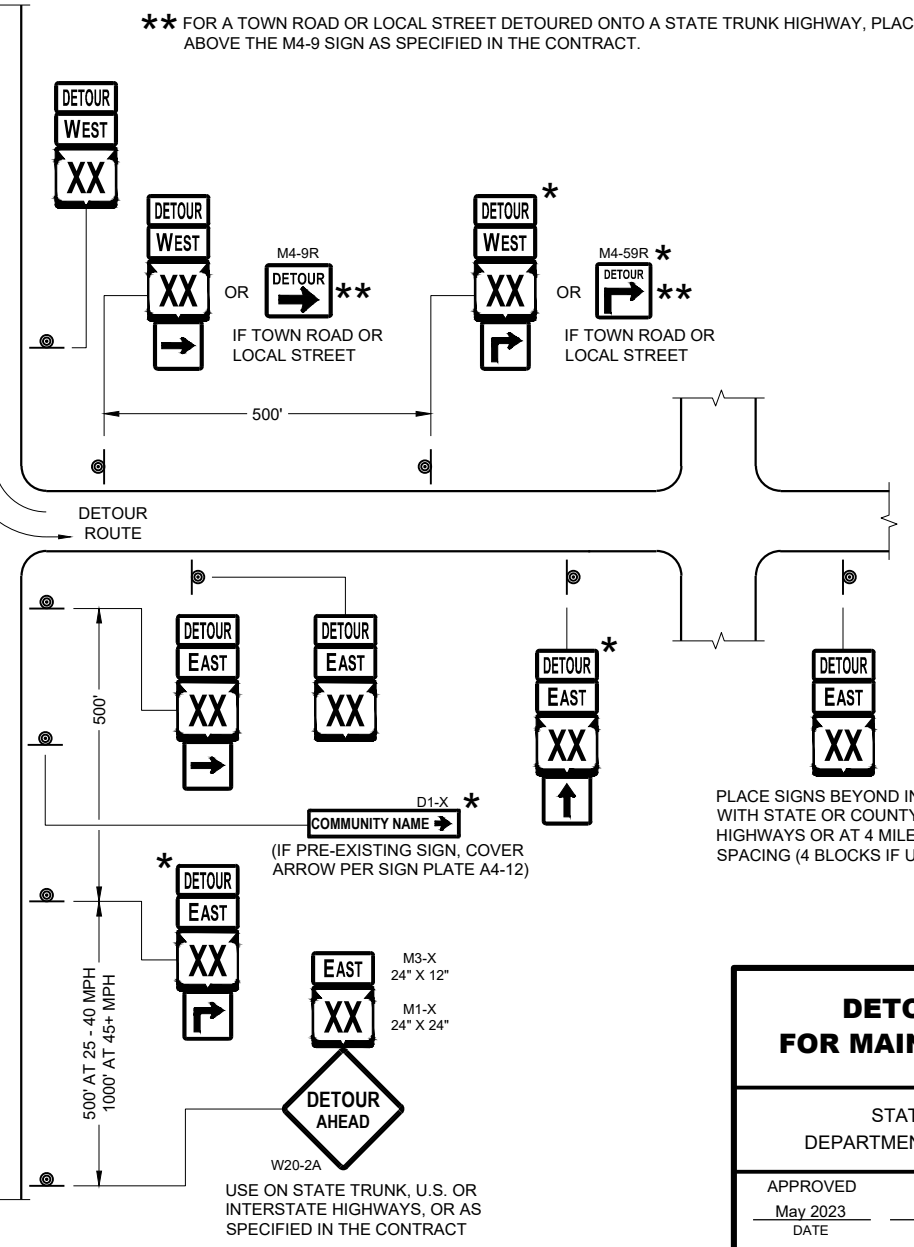
- M3-X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4-8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1-4, M1-5A AND M1-6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- M05-1 AND M06-1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- M4-9 AND M4-9R SHALL BE 30" X 24"
- M4-8a SHALL BE 24" X 18"
- G20-51 SHALL BE 60" X 24"
- W20-2A SHALL BE 48" X 48"
- D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.

- * OPTIONAL SIGNS. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS.
- ** FOR A TOWN ROAD OR LOCAL STREET DETOURED ONTO A STATE TRUNK HIGHWAY, PLACE A ROAD NAME PLAQUE ABOVE THE M4-9 SIGN AS SPECIFIED IN THE CONTRACT.

MATCH POINT



**DETAIL F
DETOUR SIGNING**



PLACE SIGNS BEYOND INTERSECTIONS WITH STATE OR COUNTY TRUNK HIGHWAYS OR AT 4 MILE MAXIMUM SPACING (4 BLOCKS IF URBAN AREA)

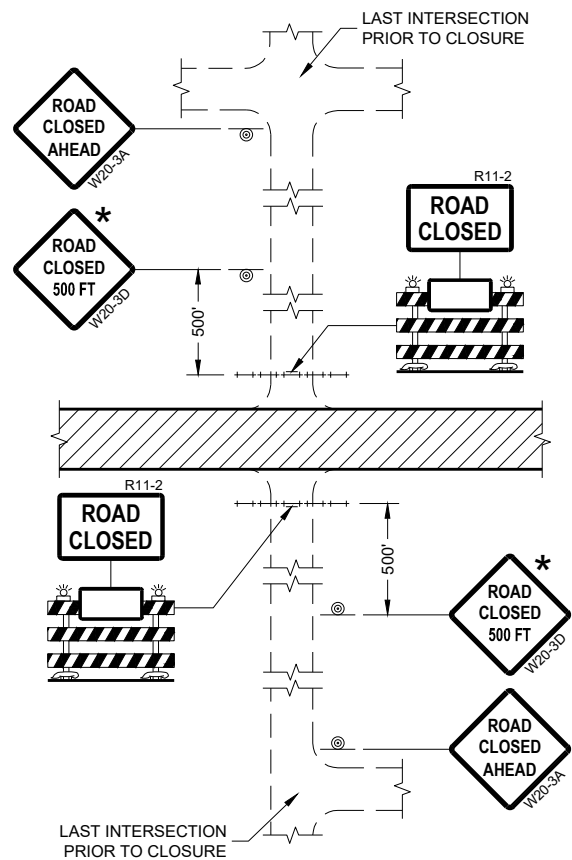
SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS AND DETAIL A OR B ON SDD SHEET 15C02 - SHEET "a"

**DETOUR SIGNING
FOR MAINLINE CLOSURES**

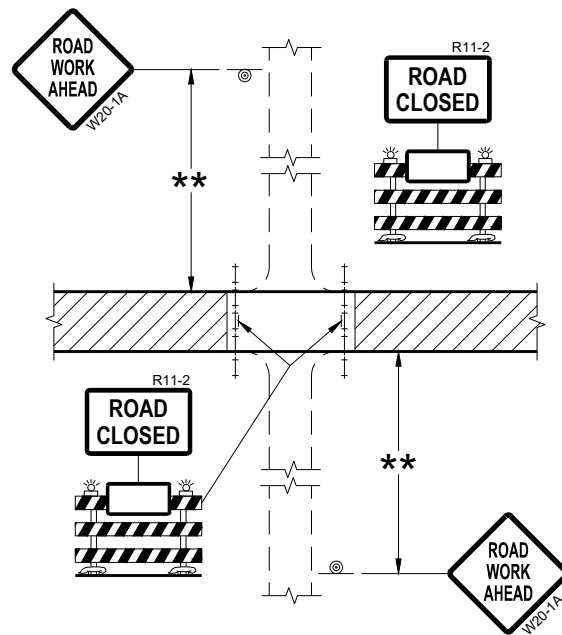
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE May 2023 /S/ Andrew Heidtke
WORK ZONE ENGINEER

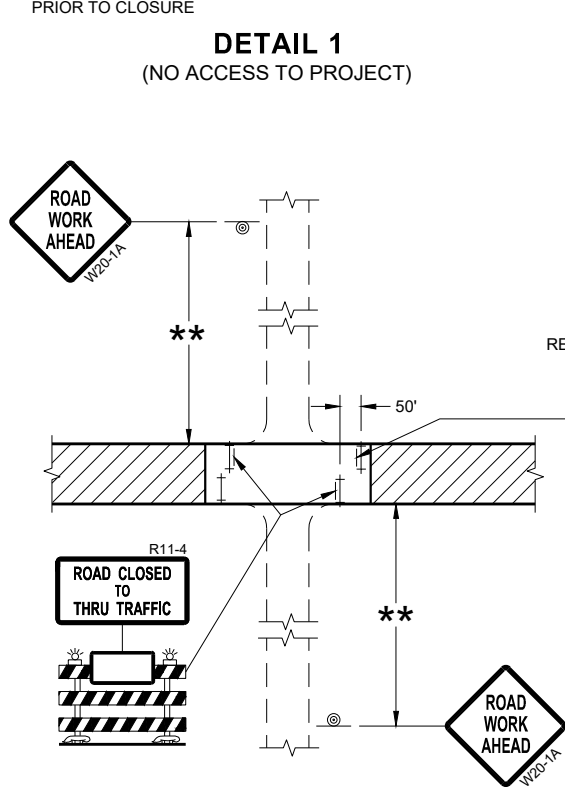
FHWA



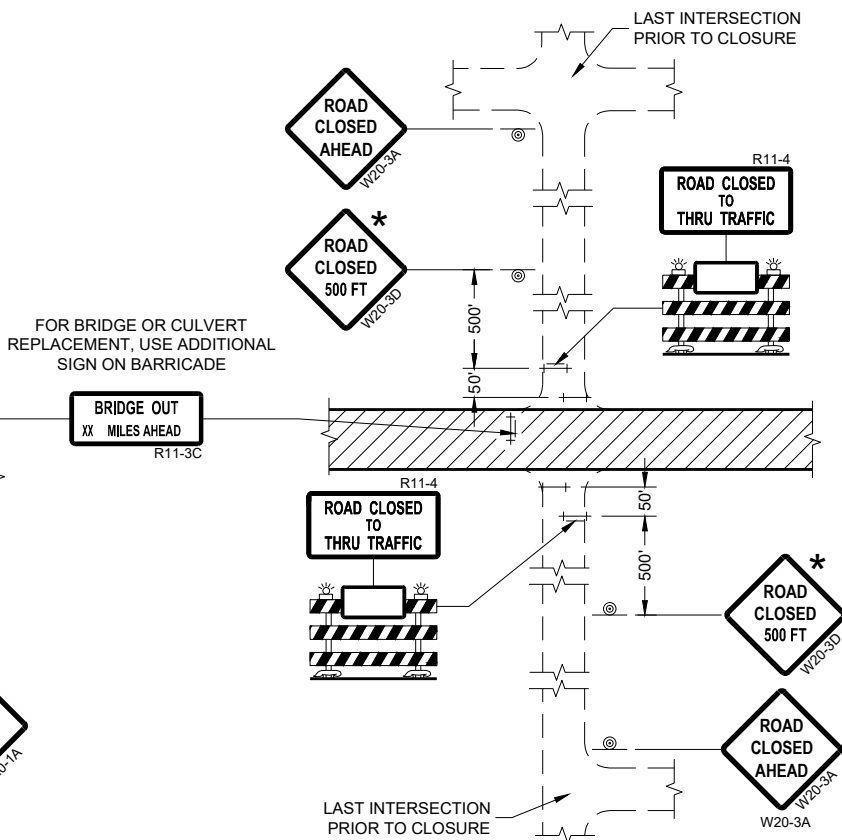
DETAIL 1
(NO ACCESS TO PROJECT)



DETAIL 2
(PUBLIC CROSS-TRAFFIC MAINTAINED.
NO ACCESS TO PROJECT)



DETAIL 3
(PUBLIC CROSS-TRAFFIC MAINTAINED.
CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)



DETAIL 4
(CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE (500 FEET DESIRABLE) TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3, AND R11-4 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
R11-2 SHALL BE 48" X 30".
R11-4 AND R11-3 SHALL BE 60" X 30".

- * OMIT THE "ROAD CLOSED 500 FT." SIGN IF THE LAST INTERSECTION IS 500 FEET OR LESS FROM THE WORK ZONE.
- ** 500' MAX. OR AT LAST INTERSECTION, WHICHEVER IS CLOSEST.

LEGEND

- ⊙ SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- ⚡ TYPE "A" WARNING LIGHT (FLASHING)
- ▨ WORK AREA

**BARRICADES AND SIGNS
FOR
SIDEROAD CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
July 2018 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

GENERAL NOTES

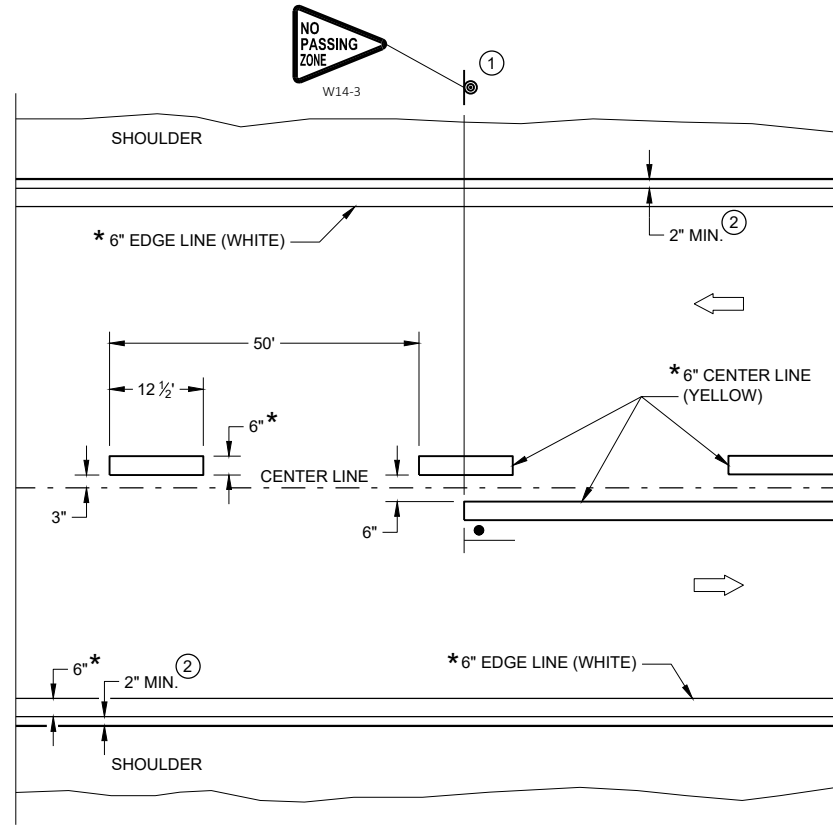
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

- ① LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING
- ② MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

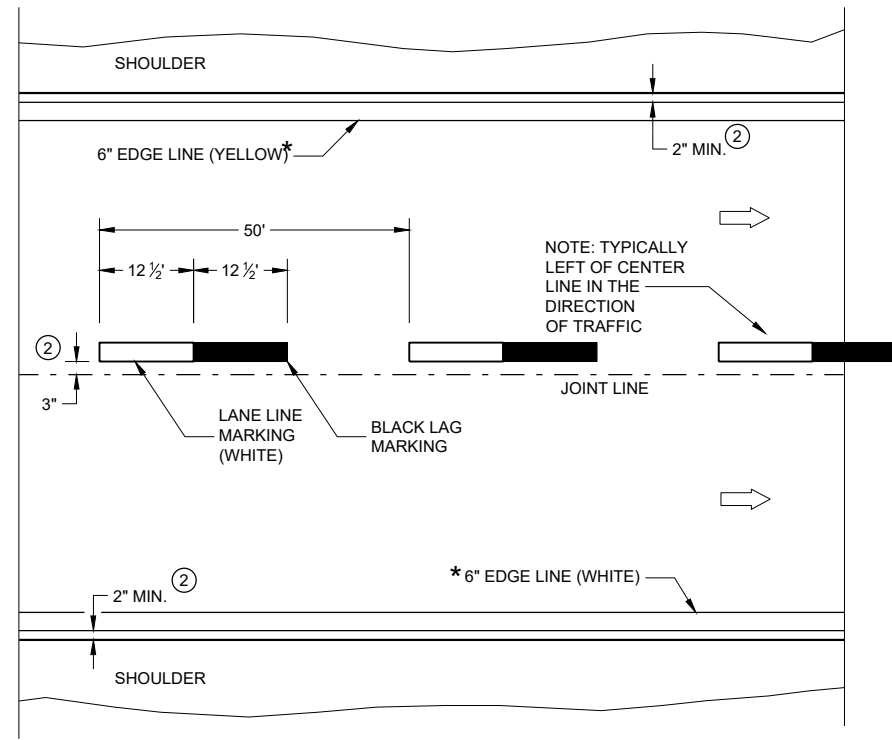
LEGEND

- "T" MARKING
- ⊙ SIGN ON PERMANENT SUPPORT
- ➡ DIRECTION OF TRAFFIC

* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES



TWO WAY TRAFFIC



ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING

6

6

SDD 15C08-23a

SDD 15C08-23a





PERMANENT LONGITUDINAL PAVEMENT MARKINGS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE: May 2023 /S/ Jeannie Silver
STATEWIDE SIGNING AND MARKING ENGINEER

FHWA

LEGEND

- V1 LEAD VEHICLE
- V2 MARKING VEHICLE
- V3 SHADOW VEHICLE
-  TRUCK MOUNTED ATTENUATOR (TMA)
-  SIGN ON TEMPORARY SUPPORT
-  DIRECTION OF TRAFFIC
-  FLASHING ARROW PANEL (CAUTION)

GENERAL NOTES

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

ALL VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL OPERATING IN CAUTION MODE. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.

DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL AND HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.

THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS.

WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR LAY STATIONARY SIGNS AND SUPPORTS FLAT ON THE GRADE WITH UPRIGHTS ORIENTED PARALLEL TO AND DOWNSTREAM FROM TRAFFIC.

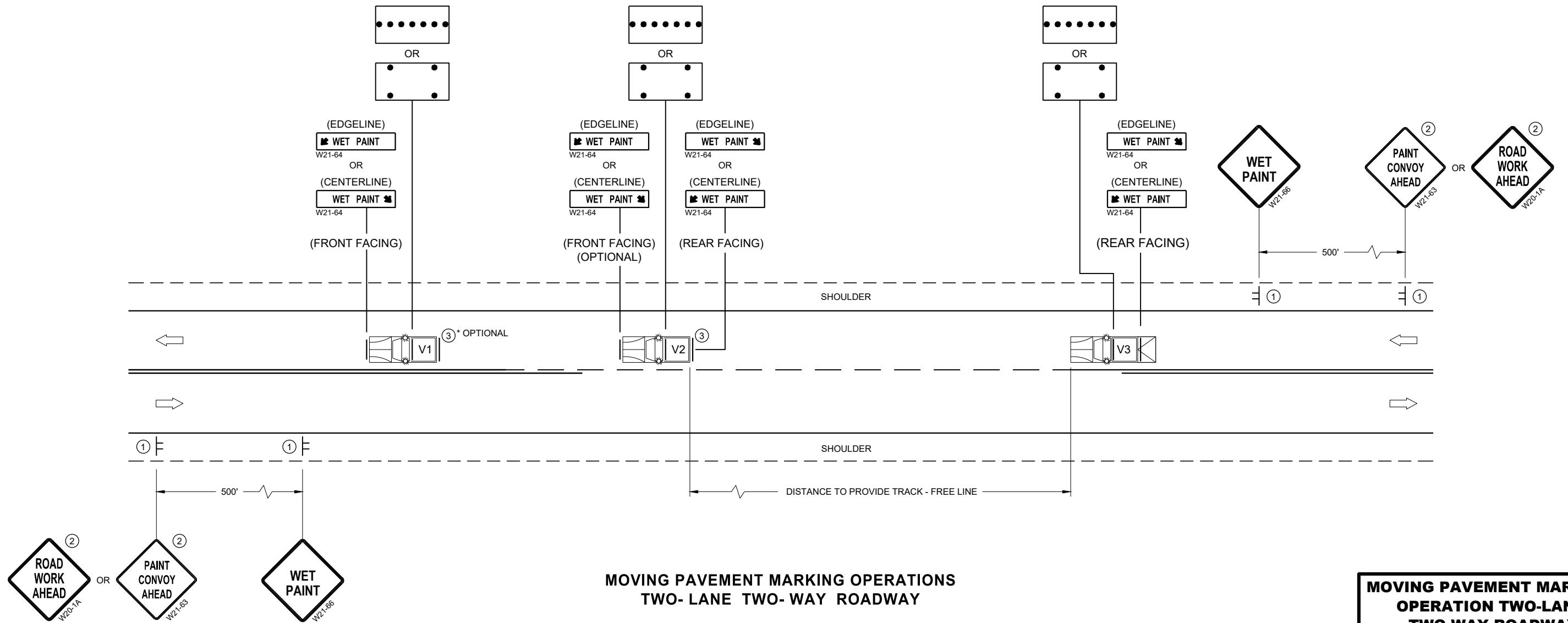
CONES SHOULD BE USED BETWEEN THE MARKING AND SHADOW VEHICLE AT 100 FOOT SPACING. CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.

CONES SHALL BE A MINIMUM OF 28" FOR WET PAVEMENT MARKING .

- ① SIGNS SHALL BE REPEATED APPROXIMATELY EVERY THREE MILES.
- ② IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1A OR W21-63 ARE NOT REQUIRED.
- ③ V1 AND V2 CAN BE SWITCHED SO THAT THE MARKER IS THE LEAD VEHICLE.

6

6

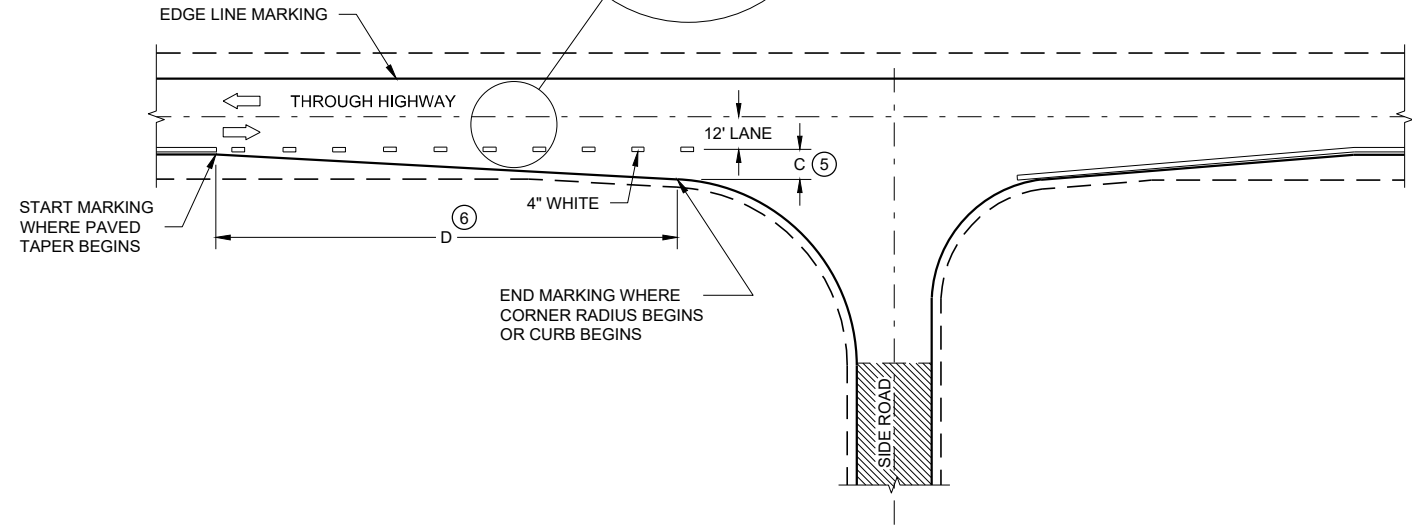
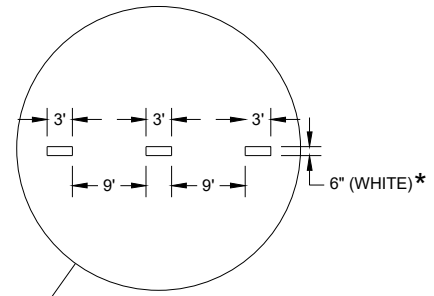


**MOVING PAVEMENT MARKING OPERATIONS
TWO-LANE TWO-WAY ROADWAY**

SDD 15C19-08a

SDD 15C19-08a

MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED February 2023 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	



MINOR INTERSECTION

* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES

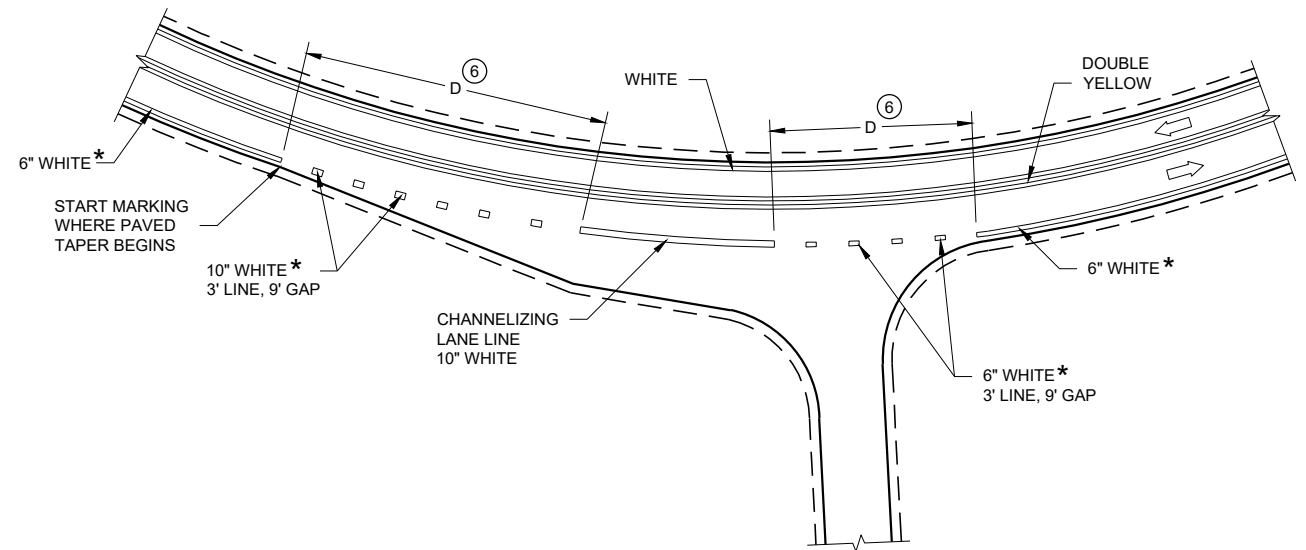
GENERAL NOTES

OMIT EDGE LINES THROUGH INTERSECTIONS. CONTINUE EDGE LINES THROUGH DRIVEWAYS.

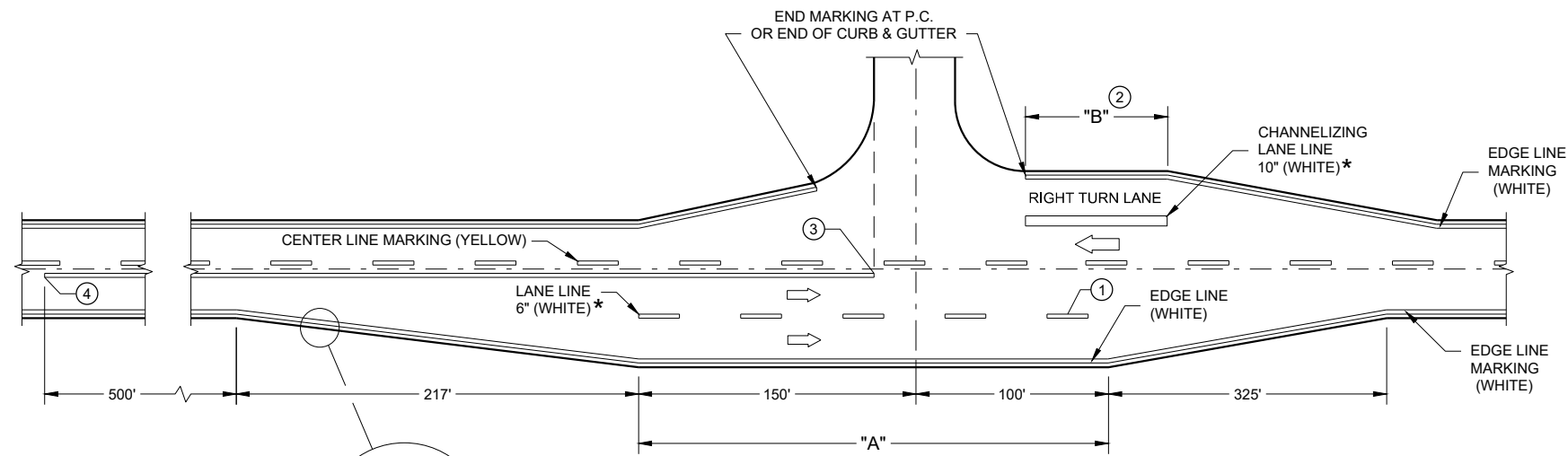
- ① WHEN DISTANCE "A" IS LESS THAN 250 FEET, OMIT LANE LINE.
- ② WHEN DISTANCE "B" IS LESS THAN 100 FEET, OMIT CHANNELIZING LANE LINE.
- ③ BARRIER LINE ENDS AT SIDE ROAD PAVEMENT / SURFACE EDGE EXTENSION.
- ④ BARRIER LINE STARTS 500 FEET PRIOR TO THE BYPASS TAPER.
- ⑤ WHEN DISTANCE "C" IS LESS THAN 4 FEET, OMIT DOTTED EXTENSION.
- ⑥ WHEN DISTANCE "D" IS LESS THAN 50 FEET, OMIT DOTTED EXTENSION.

LEGEND

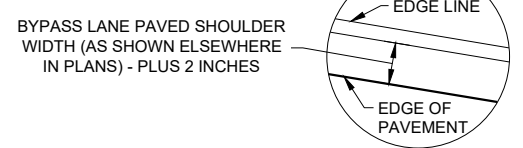
➡ DIRECTION OF TRAVEL



INTERSECTION ON OUTSIDE OF CURVE



**MAJOR INTERSECTIONS
(INTERSECTION WITH FULL RIGHT TURN LANE OR BYPASS LANE)**

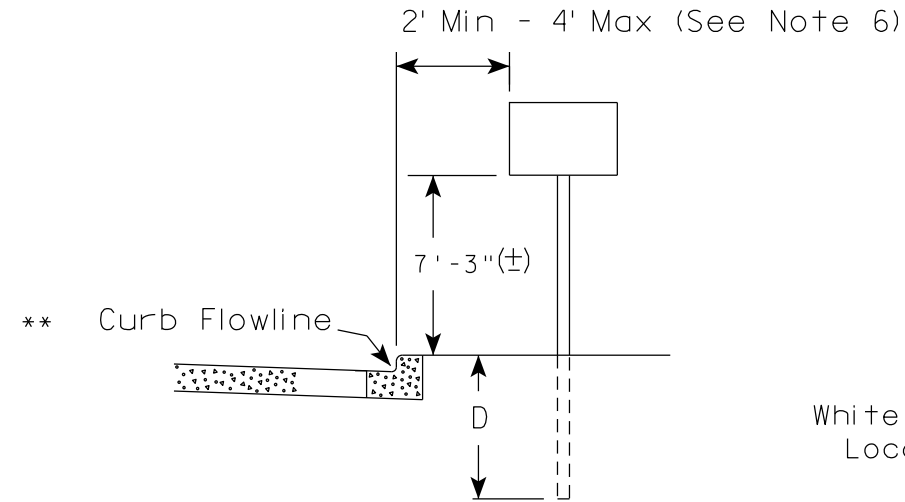


**PAVEMENT MARKING
(INTERSECTIONS)**

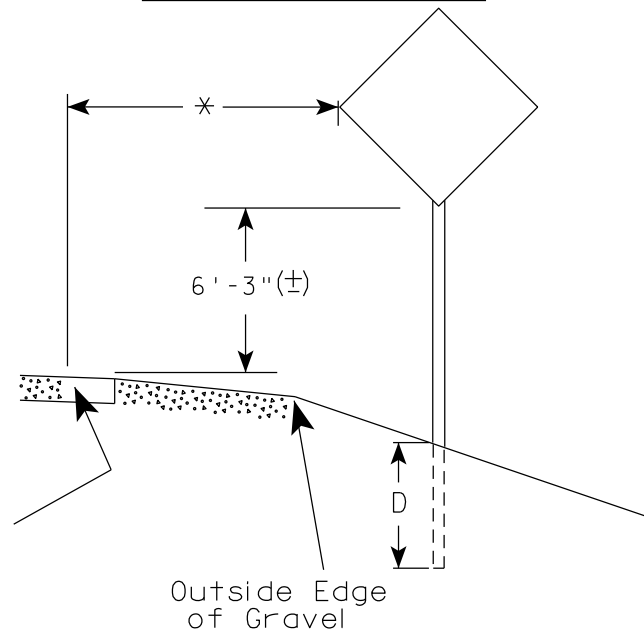
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

URBAN AREA

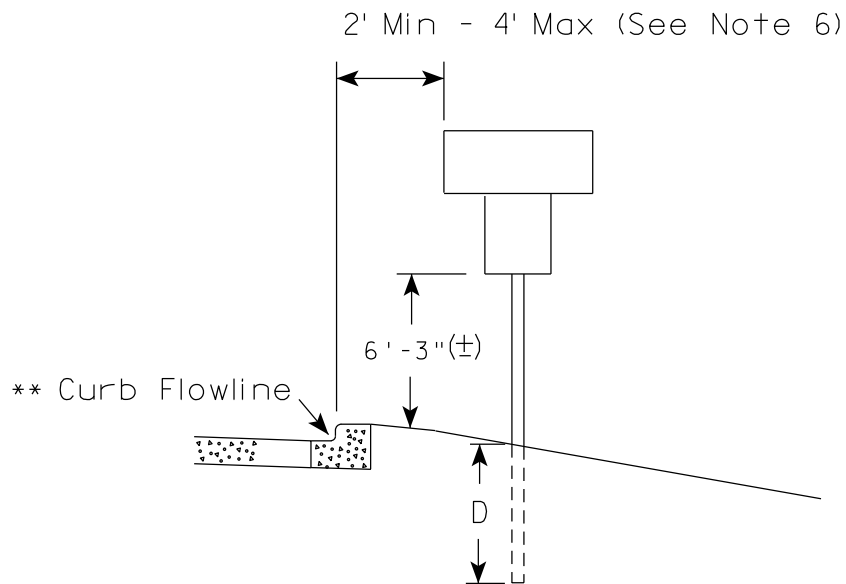
RURAL AREA (See Note 2)



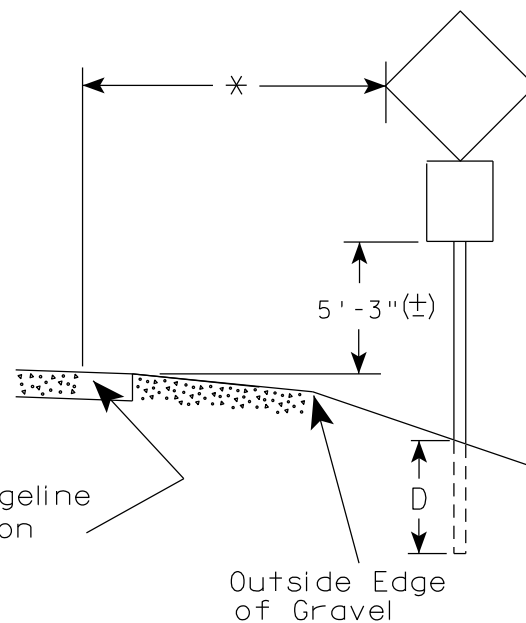
White Edgeline Location



Outside Edge of Gravel



White Edgeline Location



Outside Edge of Gravel

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

GENERAL NOTES

1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on or behind barrier wall, see A4-10 sign plate.
The Double Arrow sign (W12-1D) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).
3. For expressways and freeways, mounting height is 7'- 3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
4. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±).
5. Offset distance shall be consistent with existing signs or consistent throughout length of project.
6. The (±) tolerance for mounting height is 3 inches.
7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the Engineer.

7

7

* * The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

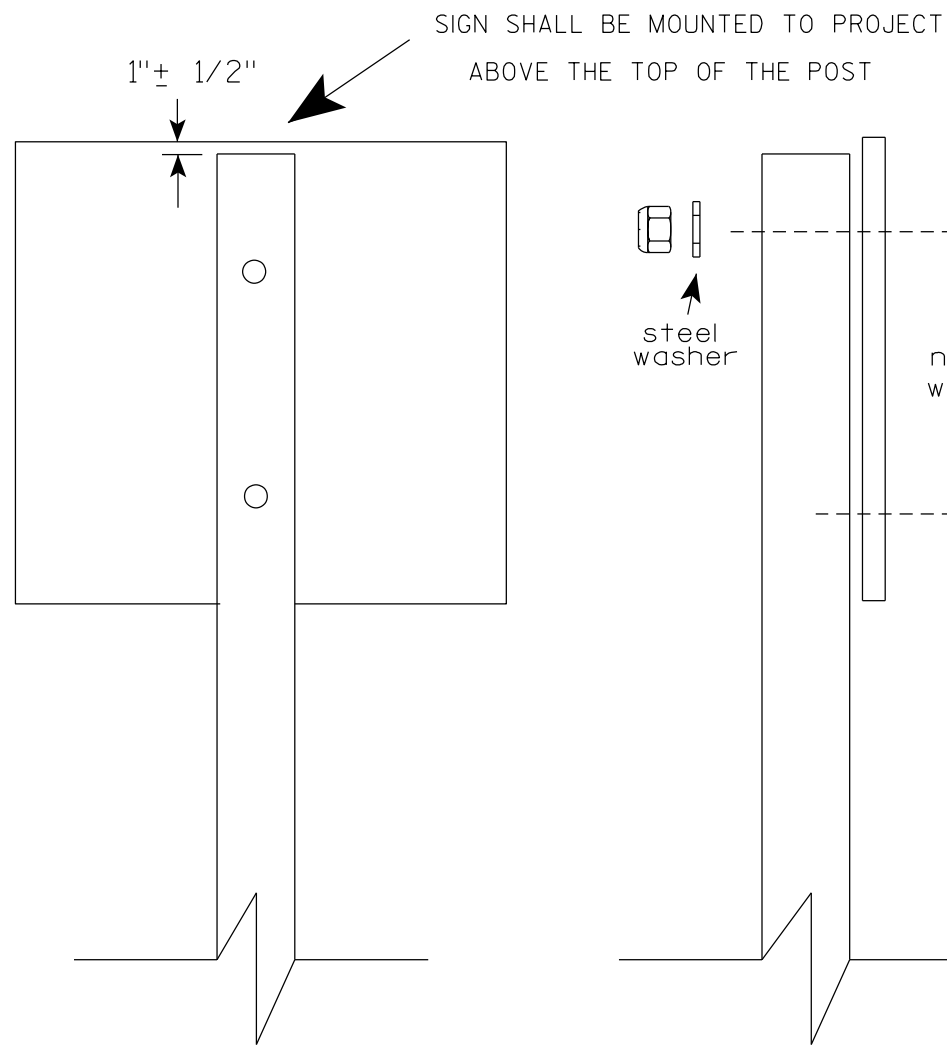
* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 5/13/2020 PLATE NO. A4-3.22



SIGN SHALL BE MOUNTED TO PROJECT
ABOVE THE TOP OF THE POST

1"± 1/2"

steel washer

nylon washer

steel washer

Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

- a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
- b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)

MACHINE BOLTS - 5/16" X 1-3/4" Length w/ lock nuts

WOOD POSTS (4" x 6")

LAG SCREWS - 3/8" X 3" (NO STRINGERS ON BACK OF SIGN)
3/8" X 4" (STRINGERS ON BACK OF SIGN)

SQUARE STEEL POSTS (2" x 2")

MACHINE BOLTS - 3/8" X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)
3/8" X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)

RIVETS - 9/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH

WASHERS (ALL POSTS) -

1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL

1-1/4" O.D. X 3/8" I.D. X .080 NYLON

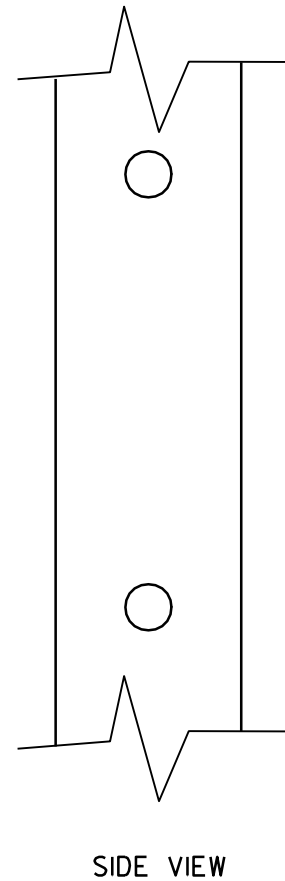
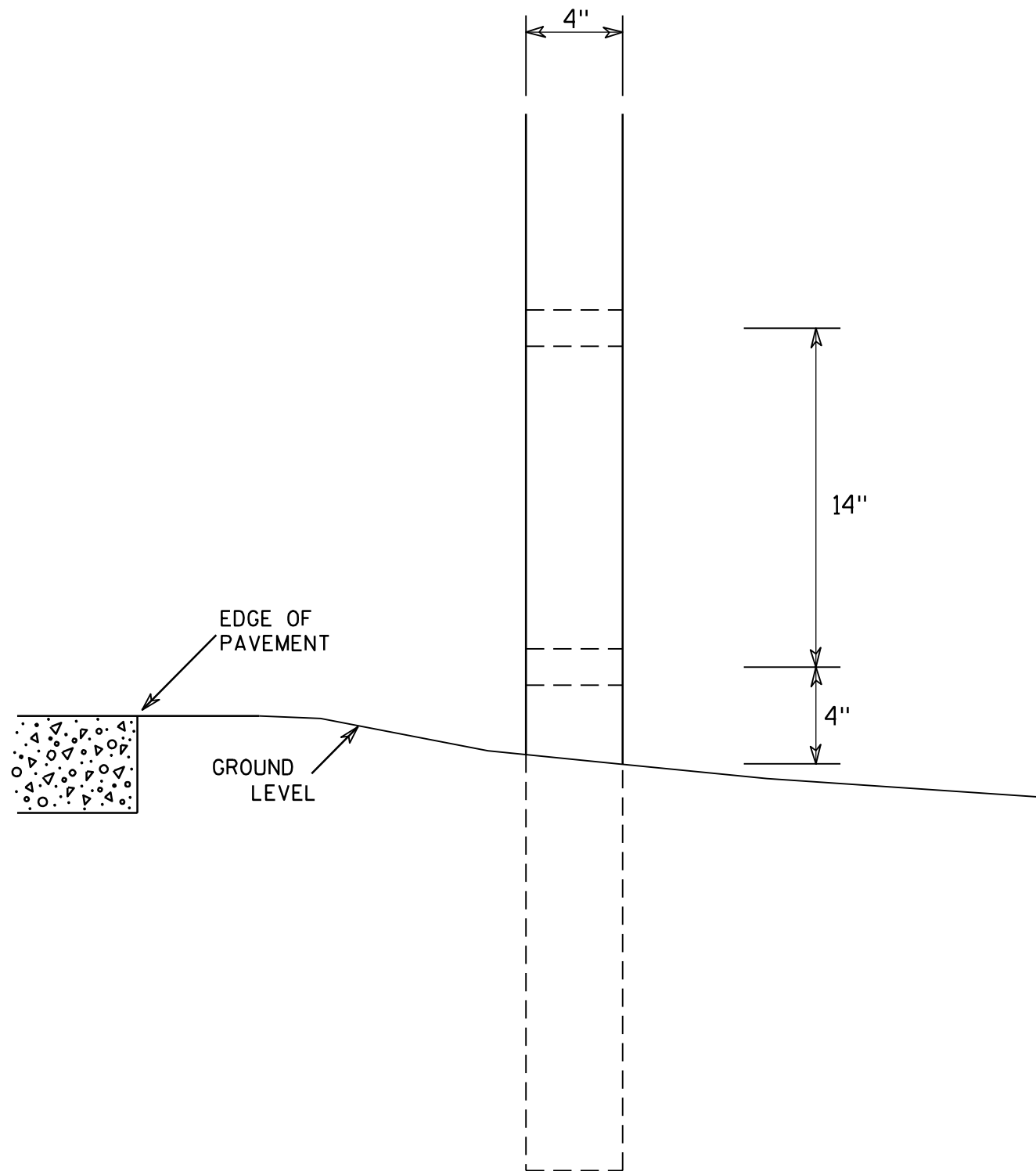
* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS
TO POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Rauch*
For State Traffic Engineer

DATE 4/1/2020 PLATE NO. A4-8.9




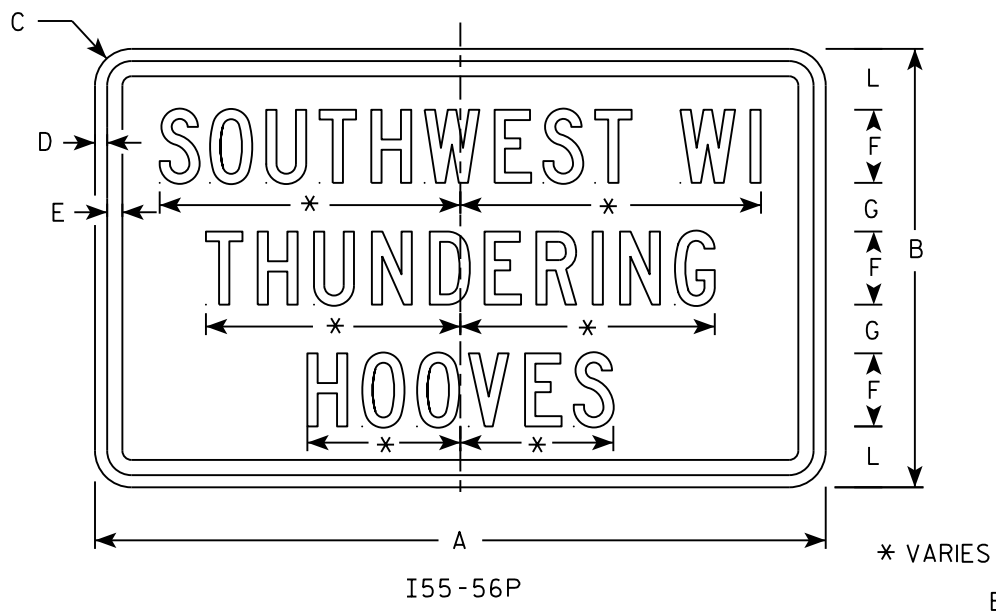
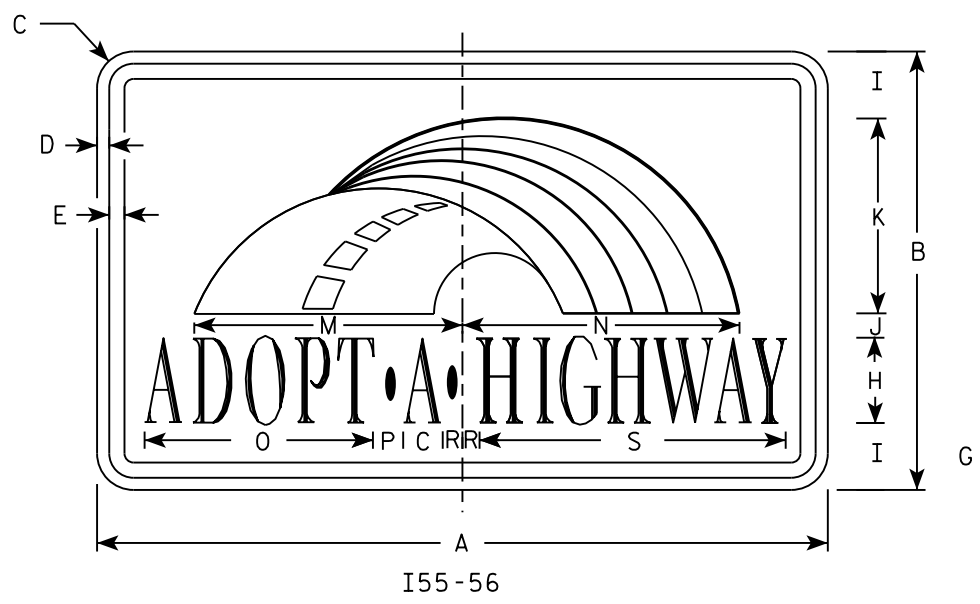
GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

7

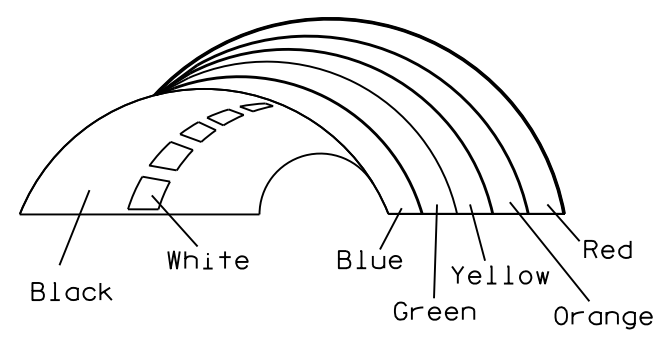
7

4 X 6 WOOD POST MODIFICATIONS	
<i>WISCONSIN DEPT OF TRANSPORTATION</i>	
APPROVED	 <small>for State Traffic Engineer</small>
DATE <u>3/27/97</u>	PLATE NO. <u>A4-11.2</u>



* VARIES

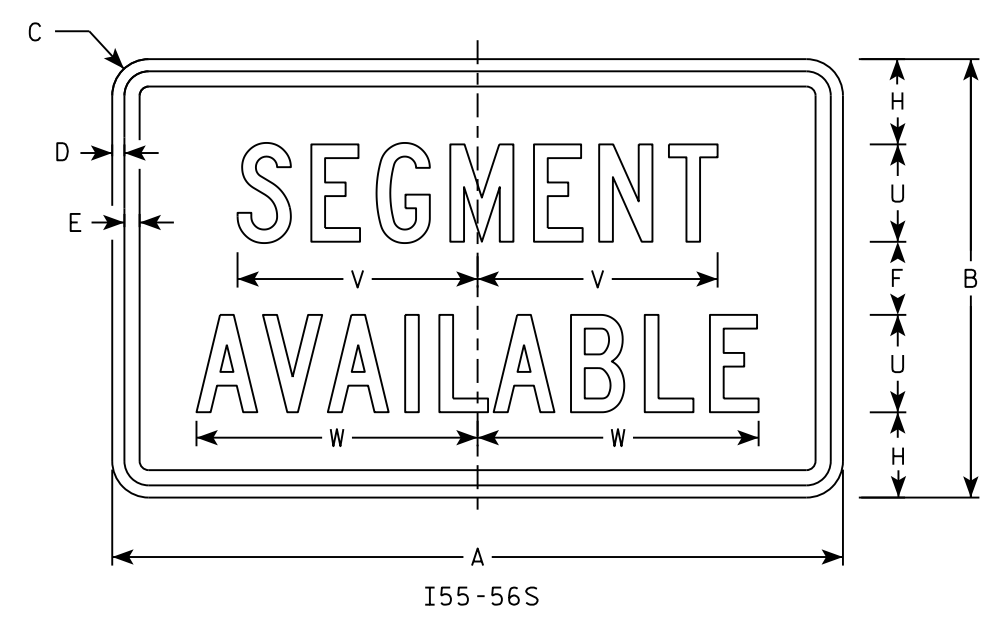
Background Colors of Symbol*



*1/4" Black Border between each color of rainbow and border of rainbow

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - (See Note 4)
3. Message Series - (See Note 5)
4. Border - Blue
Adopt a Highway - Red
All other Text - Blue
5. Adopt a Highway - Dutch 8011L
All other Text - Series C
6. Contractor shall provide and install a new post bracket in accordance with the I55-56B sign detail.



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	30	18	1 1/2	1/2	5/8	3	2	3 1/2	2 3/4	1	8	2 1/2	11 1/4	11 1/8	9 3/8	1 1/4		3/4	12 5/8	7 1/2	4	9 7/8	11 1/2				3.75
3																											
4																											
5																											

STANDARD SIGN
I55-56

WISCONSIN DEPT OF TRANSPORTATION

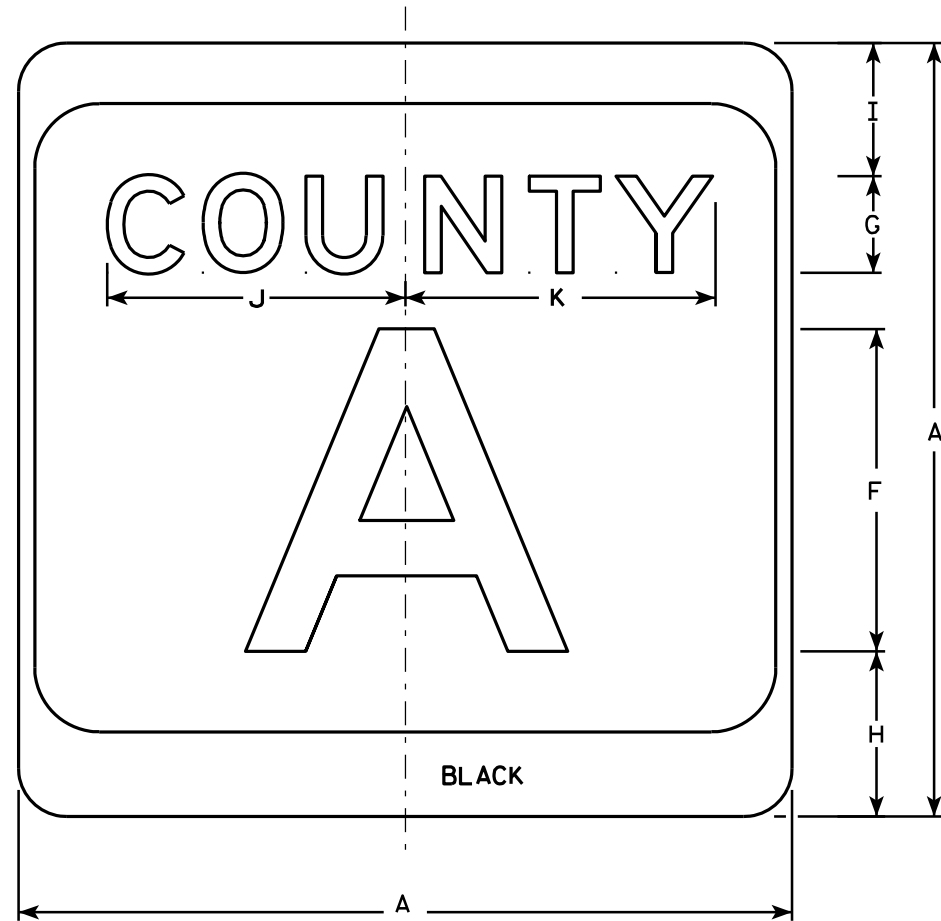
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 2/20/18 PLATE NO. I55-56.4

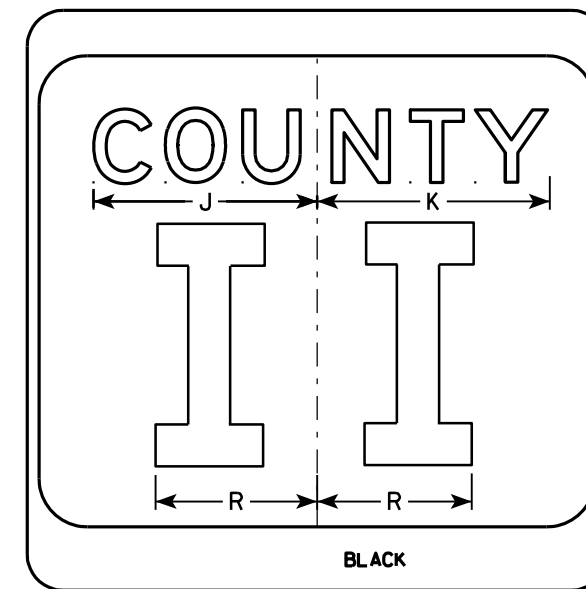
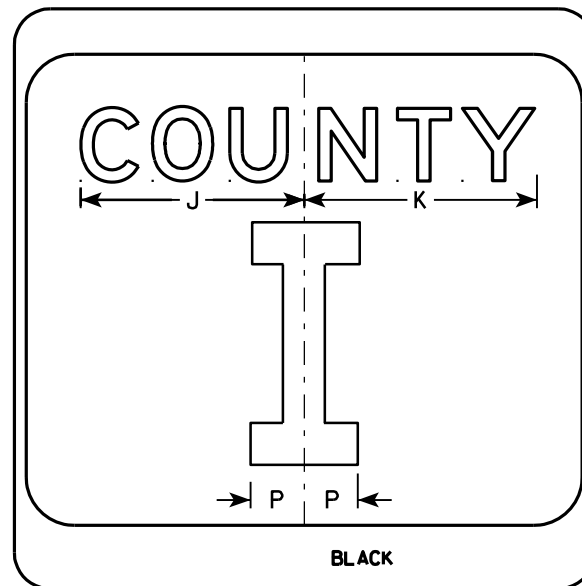
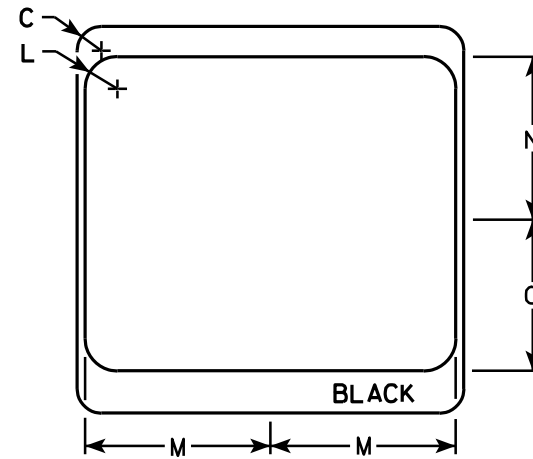
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E

NOTES

1. Sign is Type II - see Note 7 - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White & Black - See Note 7
Message - Black
3. Message Series - see Note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Message Series E for 1 letter.
Message Series D for 2 letters unless message is too big then Series C.
Message Series C for 3 letters unless message is too big then Series B.
6. Substitute appropriate letters & optically center to achieve proper balance.
7. Permanent Signs
Background - Type H Reflective
Detour or temporary Signs
Background - Reflective



M1-5A



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24		1 1/2			10	3	5 1/8	4 1/8	9 1/4	9 5/8	2	11 1/2	10 1/8	9 3/8	2 1/4		6 5/8									4.0
3	36		2 1/4			16	4	7 5/8	5 5/8	12 1/4	12 7/8	3	17 1/8	15 1/4	14	3 3/8		10									9.0
4	36		2 1/4			16	4	7 5/8	5 5/8	12 1/4	12 7/8	3	17 1/8	15 1/4	14	3 3/8		10									9.0
5	36		2 1/4			16	4	7 5/8	5 5/8	12 1/4	12 7/8	3	17 1/8	15 1/4	14	3 3/8		10									9.0

CTH MARKER
M1-5A FOR ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

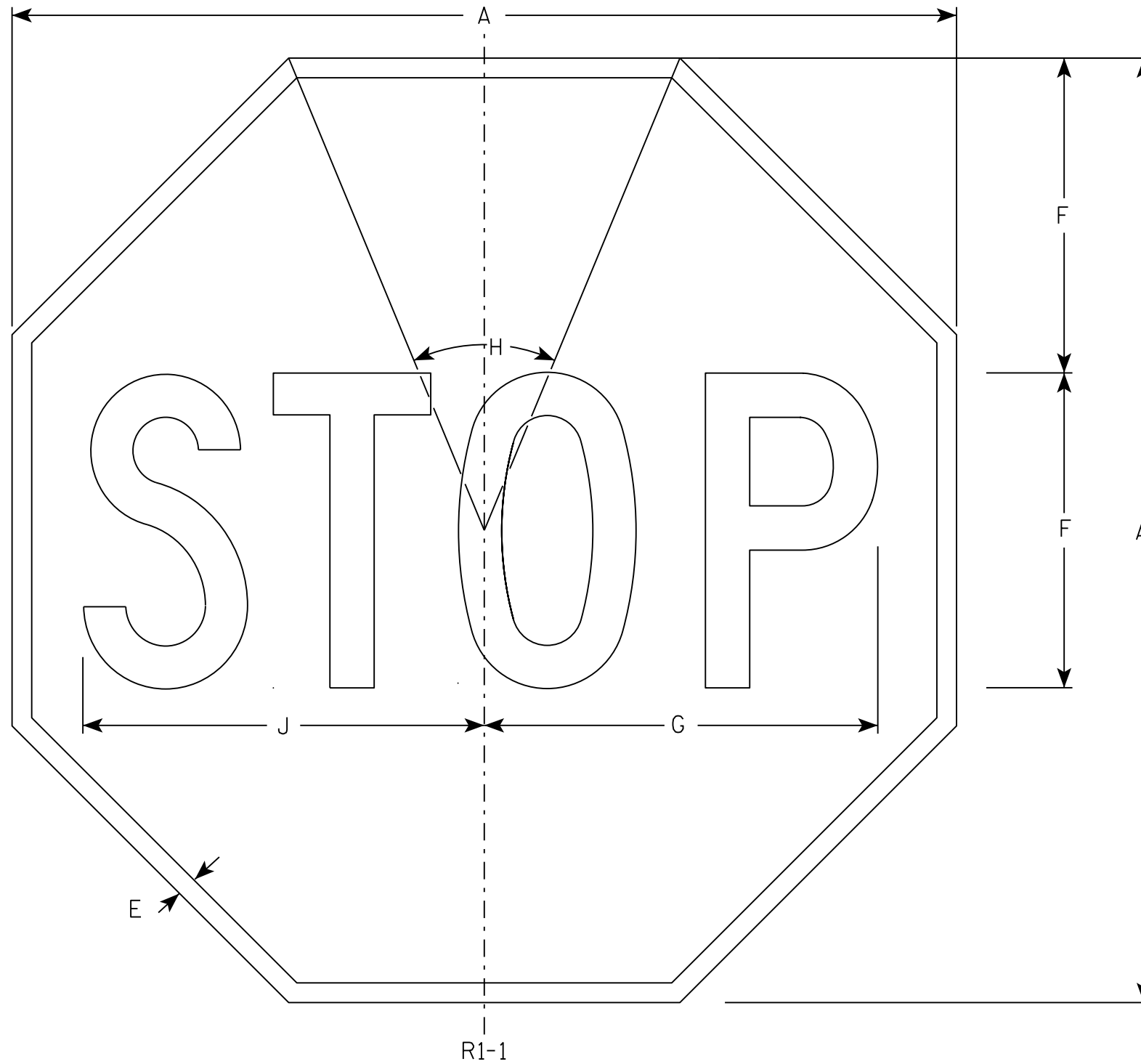
APPROVED *Matthew R. Raub*
For State Traffic Engineer

DATE 9/27/11 PLATE NO. MI-5A.8

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Red
Message - White
3. Message Series - C



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30				5/8	10	12 1/2	45°		12 3/4																	5.18
2S	30				5/8	10	12 1/2	45°		12 3/4																	5.18
2M	36				3/4	12	15	45°		15 3/8																	7.46
3	36				3/4	12	15	45°		15 3/8																	7.46
4	48				1	16	20	45°		20 1/2																	13.25
5	48				1	16	20	45°		20 1/2																	13.25
6	18				3/8	6	7 3/4	45°		7 3/4																	1.86
7	12				1/4	4	5	45°		5 1/8																	0.78

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

STANDARD SIGN
R1-1

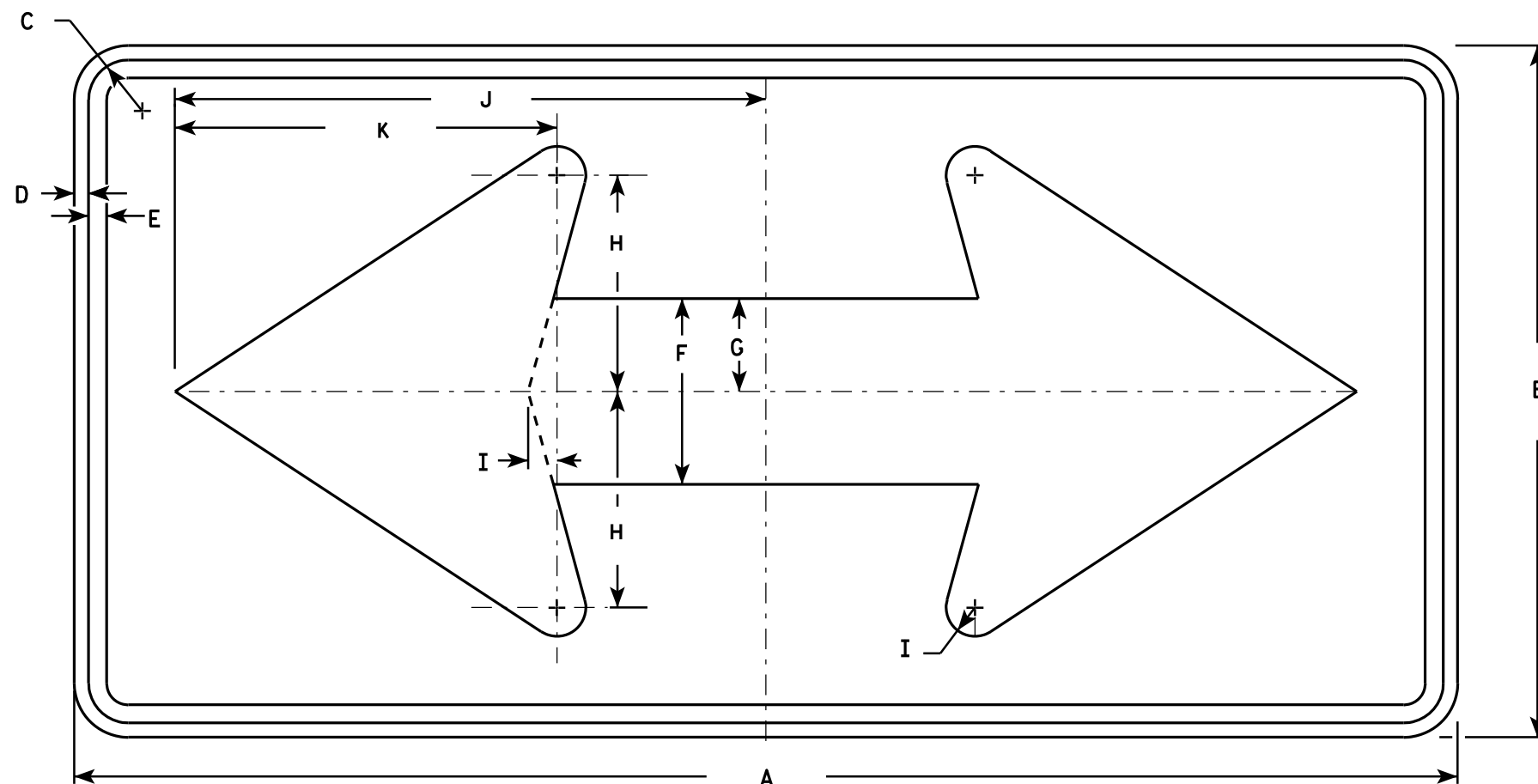
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 11/12/15 PLATE NO. R1-1.13

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



W1-7

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36	18	1 1/8	3/8	1/2	5	2 1/2	5 3/4	3/4	15 5/8	10 1/8																4.5
2S	48	24	1 3/8	1/2	5/8	6 1/2	3 1/4	7 1/2	1	20 1/2	13 1/4																8.0
2M	48	24	1 3/8	1/2	5/8	6 1/2	3 1/4	7 1/2	1	20 1/2	13 1/4																8.0
3	60	30	1 3/8	1/2	5/8	8	4	9 1/4	1 1/4	25 3/8	16 1/4																12.5
4	60	30	1 3/8	1/2	5/8	8	4	9 1/4	1 1/4	25 3/8	16 1/4																12.5
5	96	48	2 1/4	3/4	1	13	6 1/2	15	2	41	26 1/2																32.0

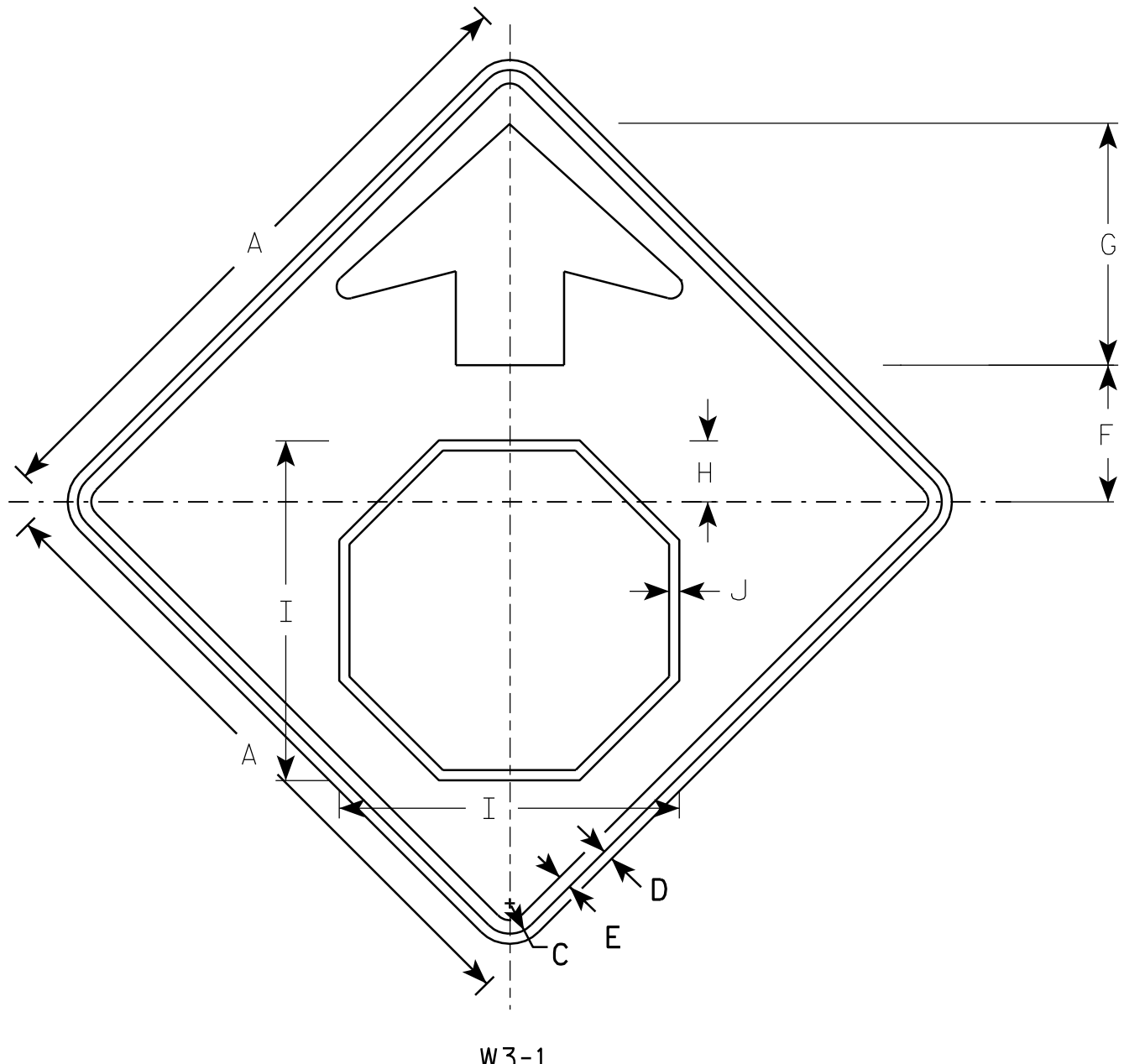
STANDARD SIGN
W1-7

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

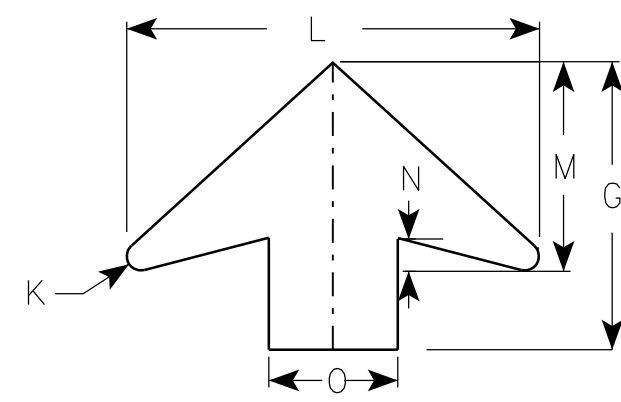
DATE 6/7/10 PLATE NO. W1-7.7

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E



NOTES

1. All Signs Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
 Background - YELLOW
 Arrow & Border - BLACK
 Stop Symbol - WHITE BORDER ON RED BACKGROUND



ARROW DETAIL

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30		1 3/8	1/2	5/8	6 1/4	11 1/4	2 7/8	15 3/4	1/2	1/2	16	8	1 1/4	5												6.25
2S	36		1 5/8	5/8	3/4	7 1/2	13 1/2	3 1/2	19	5/8	5/8	19 1/4	9 3/4	1 5/8	6												9.0
2M	36		1 5/8	5/8	3/4	7 1/2	13 1/2	3 1/2	19	5/8	5/8	19 1/4	9 3/4	1 5/8	6												9.0
3	36		1 5/8	5/8	3/4	7 1/2	13 1/2	3 1/2	19	5/8	5/8	19 1/4	9 3/4	1 5/8	6												9.0
4	48		2 1/4	3/4	1	10	17 7/8	4 1/2	25 1/8	3/4	7/8	25 5/8	13	2	8												16.0
5	48		2 1/4	3/4	1	10	17 7/8	4 1/2	25 1/8	3/4	7/8	25 5/8	13	2	8												16.0

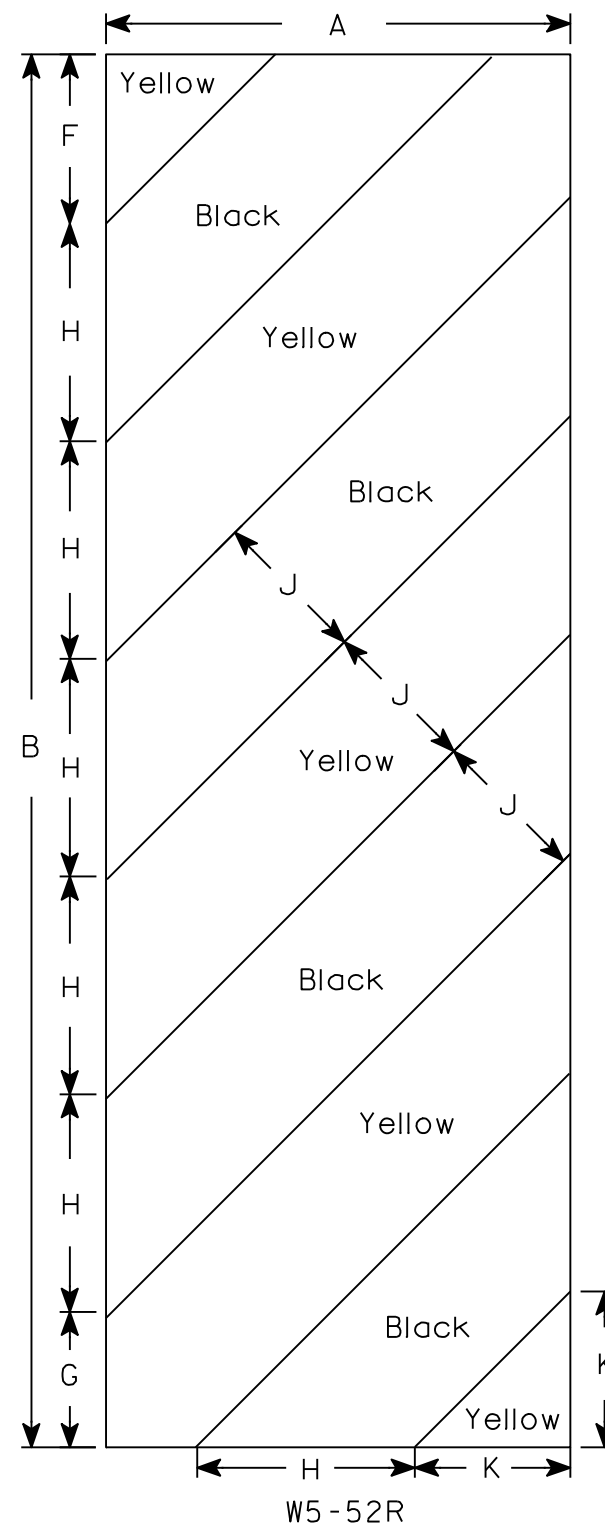
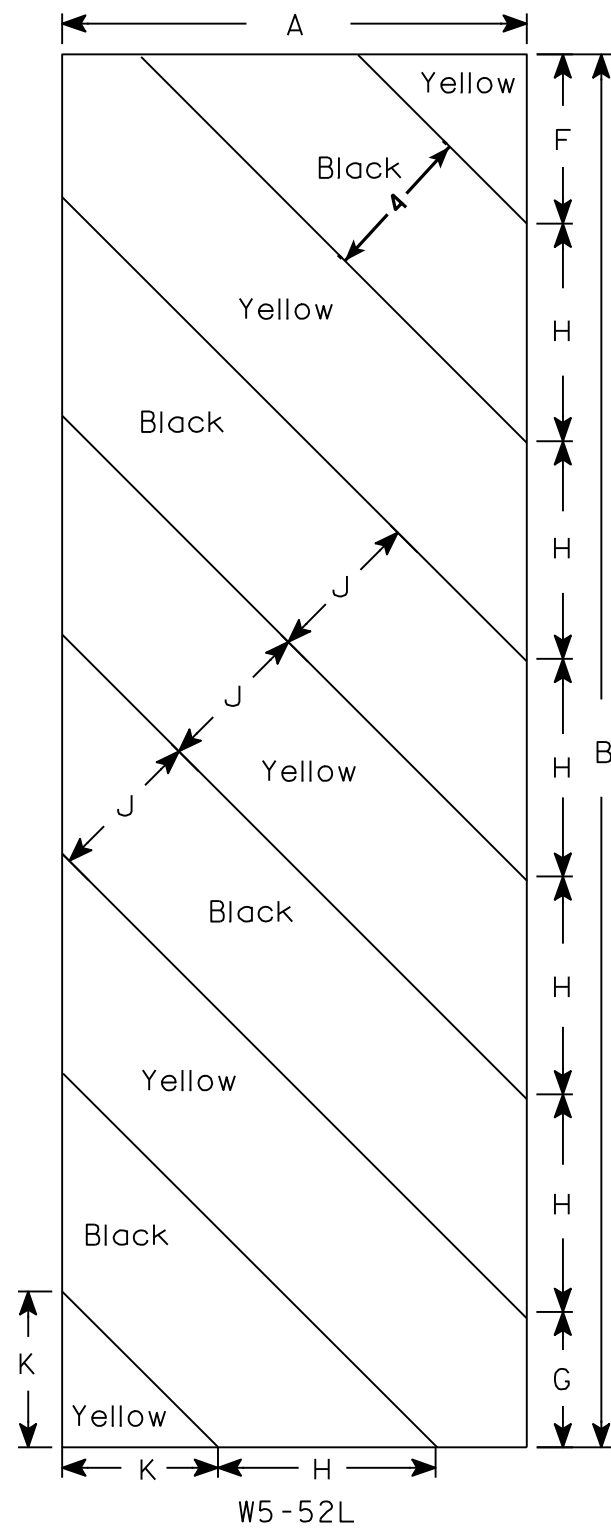
STANDARD SIGN
W3-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 6/7/10 PLATE NO. W3-1.12

PROJECT NO: _____ SHEET NO: _____ E



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. Alternate colors of stripes as shown.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	12	36				4 3/8	3 1/2	5 5/8	45°	4	4																3.0
2M	12	36				4 3/8	3 1/2	5 5/8	45°	4	4																3.0
3	18	54				6	5 1/2	8 1/2	45°	6	6 9/16																6.75
4																											
5																											

STANDARD SIGN
W5-52L & W5-52R

WISCONSIN DEPT OF TRANSPORTATION

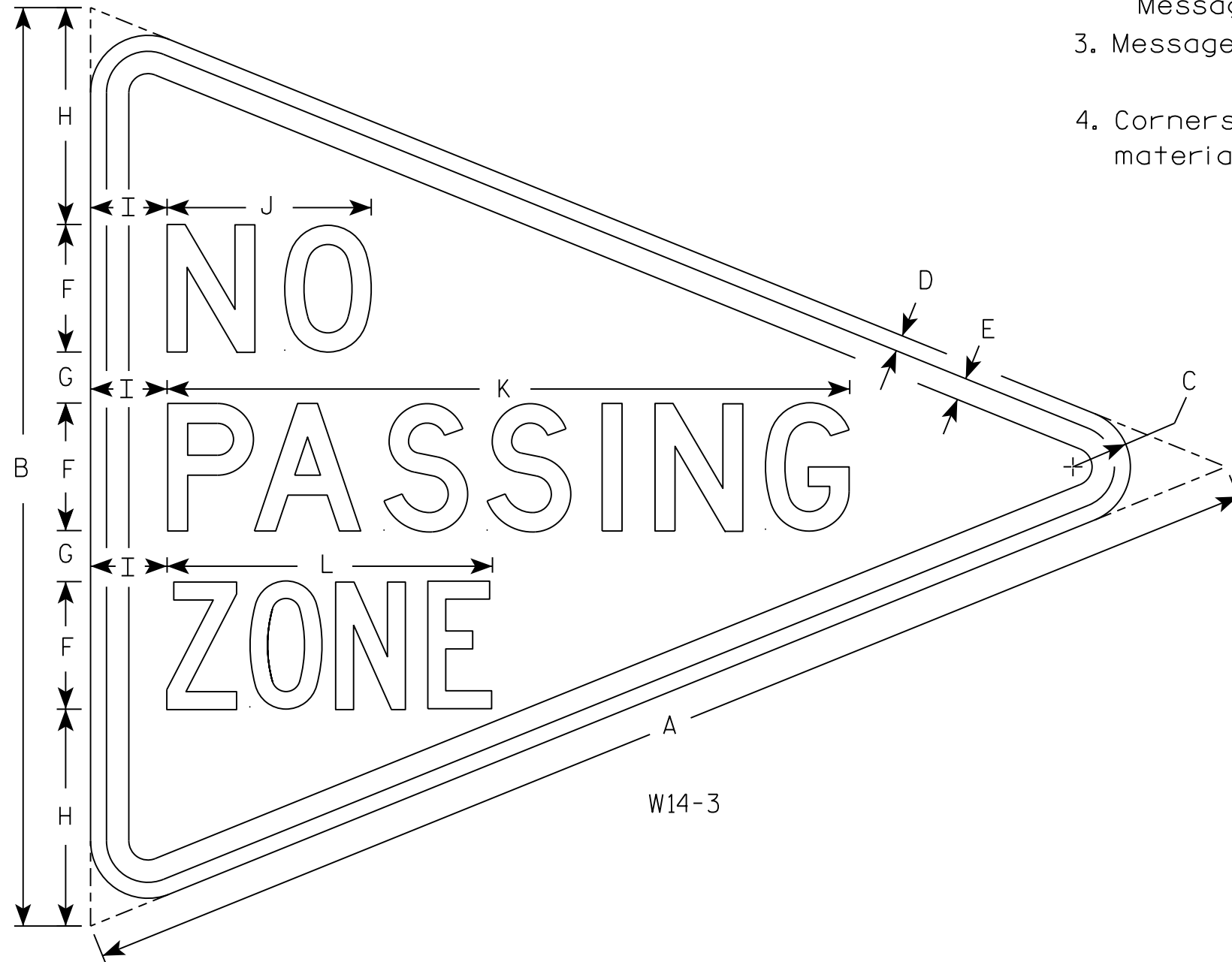
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 5/29/12 PLATE NO. W5-52.9

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Yellow
Message - Black
3. Message Series - Lines 1 and 2 are Series D.
Line 3 is series C.
4. Corners and borders shall be rounded on all base materials for this sign.



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48	36	2 1/4	5/8	7/8	5	2	8 1/2	3	8	26 3/4	12 3/4															5.56
2M																											
3																											
4																											
5																											

STANDARD SIGN
W14-3

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 3/21/17 PLATE NO. W14-3.10

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

DIVISION 1 - CTH K

STATION	DISTANCE	AREA (SF)		INCRFMNTAL VOL (CY) (UNADJUSTED)		CUMULATIVE VOL (CY)		
		CUT	FILL	CUT	FILL	CUT	EXPANDED FILL	MASS ORDNATE
10+75.00	0.00	0.00	1.79	0	0	0	0	0
11+00.00	25.00	0.00	2.29	0	2	0	3	-3
11+88.29	88.29	0.00	2.01	0	7	0	11	-11
13+00.00	111.71	0.00	4.01	0	12	0	26	-26
13+24.09	24.09	0.00	3.86	0	4	0	31	-31
14+00.00	75.91	0.00	4.29	0	11	0	45	-45
15+00.00	100.00	0.00	11.03	0	28	0	80	-80
16+00.00	100.00	0.00	8.91	0	37	0	126	-126
17+00.00	100.00	0.00	8.29	0	32	0	166	-166
18+00.00	100.00	0.00	14.70	0	43	0	220	-220
18+83.72	83.72	0.00	24.20	0	60	0	295	-295
18+89.90	6.18	0.00	23.46	0	5	0	301	-301
19+08.70	18.80	0.00	23.82	0	16	0	321	-321
19+14.89	6.19	0.00	24.64	0	6	0	329	-329
19+33.68	18.79	0.00	19.09	0	15	0	348	-348
19+39.87	6.19	0.00	19.37	0	4	0	353	-353
20+00.00	60.13	0.00	28.58	0	53	0	419	-419
21+00.00	100.00	0.00	11.34	0	74	0	511	-511
21+27.99	27.99	0.00	12.13	0	12	0	526	-526
21+33.14	5.15	0.00	12.51	0	2	0	529	-529
21+52.98	19.84	0.00	20.82	0	12	0	544	-544
21+58.13	5.15	0.00	21.94	0	4	0	549	-549
21+77.96	19.83	0.00	26.64	0	18	0	571	-571
21+83.11	5.15	0.00	25.86	0	5	0	578	-578
22+00.00	16.89	0.00	18.91	0	14	0	595	-595
23+00.00	100.00	0.00	0.60	0	36	0	640	-640
24+00.00	100.00	0.00	5.14	0	11	0	654	-654
25+00.00	100.00	0.00	9.73	0	28	0	689	-689
26+00.00	100.00	0.00	13.44	0	43	0	743	-743
26+85.86	85.86	0.00	7.91	0	34	0	785	-785
27+00.00	14.14	0.00	8.03	0	4	0	790	-790
28+00.00	100.00	0.00	12.63	0	38	0	838	-838
29+00.00	100.00	0.00	3.94	0	31	0	876	-876
30+00.00	100.00	0.00	4.51	0	16	0	896	-896
31+00.00	100.00	0.00	5.58	0	19	0	920	-920
31+66.35	66.35	0.00	2.57	0	10	0	933	-933
32+00.00	33.65	0.00	4.05	0	4	0	938	-938
33+00.00	100.00	0.00	7.20	0	21	0	964	-964
33+74.93	74.93	0.00	5.49	0	18	0	986	-986
34+00.00	25.07	0.00	9.71	0	7	0	995	-995
35+00.00	100.00	0.00	4.01	0	25	0	1,026	-1,026
36+00.00	100.00	0.00	2.75	0	13	0	1,043	-1,043
36+23.44	23.44	0.00	0.60	0	1	0	1,044	-1,044
36+37.58	14.14	0.00	0.65	0	0	0	1,044	-1,044
37+00.00	62.42	0.00	11.78	0	14	0	1,061	-1,061

DIVISION 1 - CTH K

STATION	DISTANCE	AREA (SF)		INCRFMNTAL VOL (CY) (UNADJUSTED)		CUMULATIVE VOL (CY)		
		CUT	FILL	CUT	FILL	CUT	EXPANDED FILL	MASS ORDNATE
38+00.00	100.00	0.00	12.19	0	43	0	1,115	-1,115
38+42.83	42.83	0.00	3.90	0	13	0	1,131	-1,131
39+00.00	57.17	0.00	6.49	0	11	0	1,145	-1,145
40+00.00	100.00	0.00	15.55	0	41	0	1,196	-1,196
41+00.00	100.00	0.00	16.02	0	58	0	1,269	-1,269
42+00.00	100.00	0.00	9.21	0	47	0	1,328	-1,328
43+00.00	100.00	0.00	11.44	0	38	0	1,375	-1,375
44+00.00	100.00	0.00	11.84	0	43	0	1,429	-1,429
45+00.00	100.00	0.00	7.99	0	37	0	1,475	-1,475
46+00.00	100.00	0.00	10.75	0	35	0	1,519	-1,519
47+00.00	100.00	0.00	22.38	0	61	0	1,595	-1,595
48+00.00	100.00	0.00	15.37	0	70	0	1,683	-1,683
49+00.00	100.00	0.00	18.07	0	62	0	1,760	-1,760
49+63.44	63.44	0.00	9.05	0	32	0	1,800	-1,800
50+00.00	36.56	0.00	8.80	0	12	0	1,815	-1,815
50+35.23	35.23	0.00	3.94	0	8	0	1,825	-1,825
51+00.00	64.77	0.00	8.60	0	15	0	1,844	-1,844
52+00.00	100.00	0.00	7.07	0	29	0	1,880	1,880
53+00.00	100.00	0.00	9.08	0	30	0	1,918	-1,918
54+00.00	100.00	0.00	9.02	0	34	0	1,960	-1,960
55+00.00	100.00	0.00	8.17	0	32	0	2,000	2,000
56+00.00	100.00	0.00	6.44	0	27	0	2,034	-2,034
57+00.00	100.00	0.00	14.58	0	39	0	2,083	-2,083
58+00.00	100.00	0.00	20.62	0	65	0	2,164	-2,164
59+00.00	100.00	0.00	11.49	0	59	0	2,238	-2,238
60+00.00	100.00	0.00	8.75	0	37	0	2,284	-2,284
61+00.00	100.00	0.00	4.89	0	25	0	2,315	-2,315
62+00.00	100.00	0.00	7.05	0	22	0	2,343	-2,343
63+00.00	100.00	0.00	12.13	0	36	0	2,388	-2,388
64+00.00	100.00	0.00	9.08	0	39	0	2,436	-2,436
64+33.99	33.99	0.00	5.53	0	9	0	2,448	-2,448
65+00.00	66.01	0.00	11.49	0	21	0	2,474	-2,474
66+00.00	100.00	0.00	5.98	0	32	0	2,514	-2,514
66+42.74	42.74	0.00	3.40	0	7	0	2,523	2,523
67+00.00	57.26	0.00	8.90	0	13	0	2,539	-2,539
68+00.00	100.00	0.00	12.44	0	40	0	2,589	-2,589
69+00.00	100.00	0.00	8.72	0	39	0	2,638	-2,638
70+00.00	100.00	0.00	11.03	0	37	0	2,684	-2,684
71+00.00	100.00	0.00	14.53	0	47	0	2,743	-2,743
72+00.00	100.00	0.00	7.25	0	40	0	2,793	2,793
72+50.10	50.10	0.00	4.58	0	11	0	2,806	-2,806
73+00.00	49.90	0.00	3.82	0	8	0	2,816	-2,816
74+00.00	100.00	0.00	19.25	0	43	0	2,870	-2,870
75+00.00	100.00	0.00	25.36	0	83	0	2,974	-2,974
76+00.00	100.00	0.00	14.56	0	74	0	3,066	-3,066

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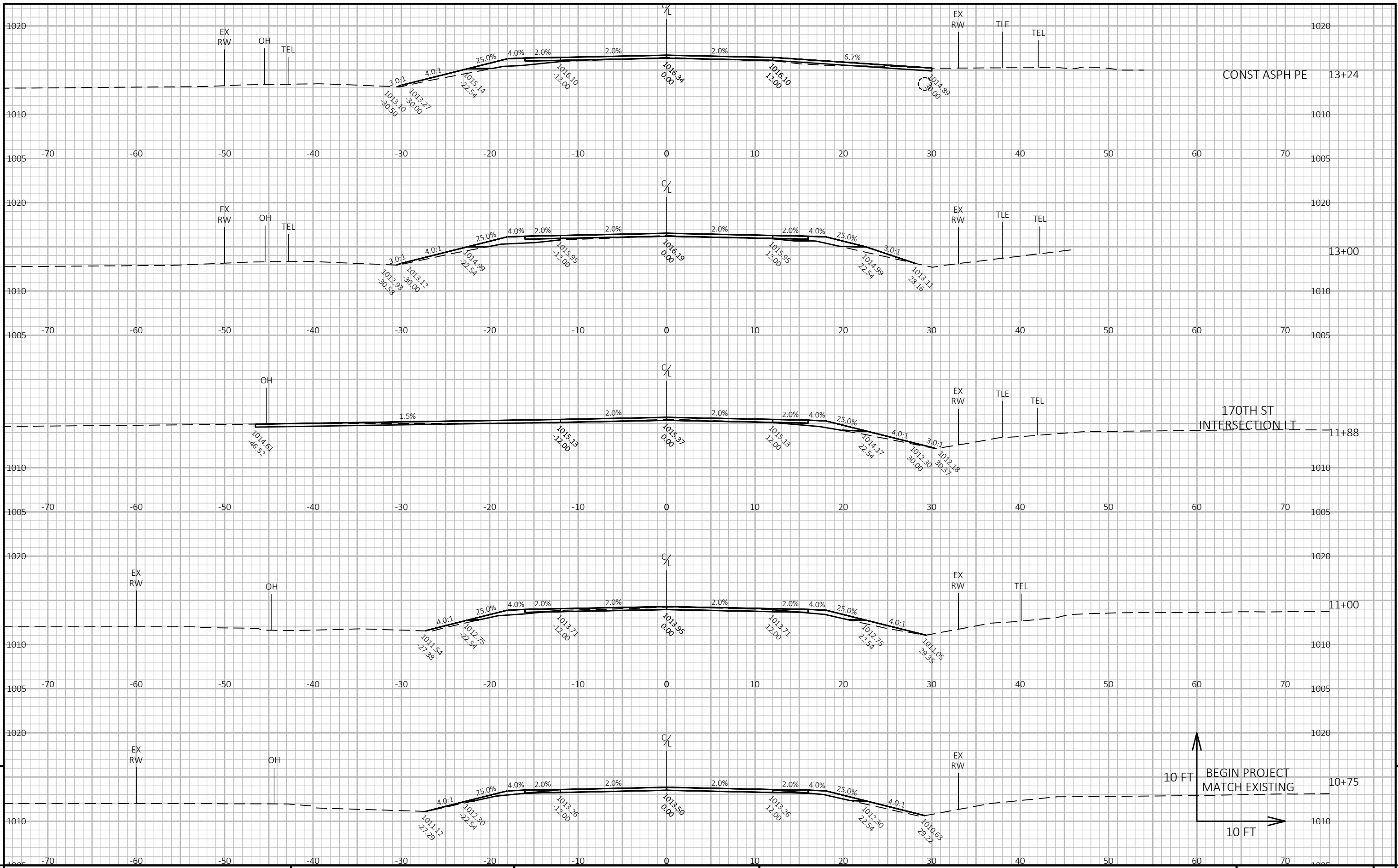
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DIVISION 1 - CTH K

STATION	DISTANCE	AREA (SF)		INCRFMNTAL VOL (CY) (UNADJUSTED)		CUMULATIVE VOL (CY)		
		CUT	FILL	CUT	FILL	CUT	EXPANDED FILL	MASS ORDINATE
77+00.00	100.00	0.00	29.76	0	82	0	3,169	-3,169
78+00.00	100.00	0.00	13.46	0	80	0	3,269	-3,269
78+23.15	23.15	0.00	15.24	0	12	0	3,284	-3,284
79+00.00	76.85	0.00	11.91	0	39	0	3,333	-3,333
80+00.00	100.00	0.00	12.25	0	45	0	3,389	-3,389
81+00.00	100.00	0.00	12.45	0	46	0	3,446	-3,446
82+00.00	100.00	0.00	12.26	0	46	0	3,504	-3,504
83+00.00	100.00	0.00	23.88	0	67	0	3,588	-3,588
84+00.00	100.00	0.00	30.04	0	100	0	3,713	-3,713
85+00.00	100.00	0.00	24.39	0	101	0	3,839	-3,839
86+00.00	100.00	0.00	25.07	0	92	0	3,954	-3,954
87+00.00	100.00	0.00	21.15	0	86	0	4,061	-4,061
88+00.00	100.00	0.00	19.85	0	76	0	4,156	-4,156
89+00.00	100.00	0.00	16.07	0	67	0	4,240	-4,240
89+53.52	53.52	0.00	13.69	0	30	0	4,278	-4,278

NOTES:

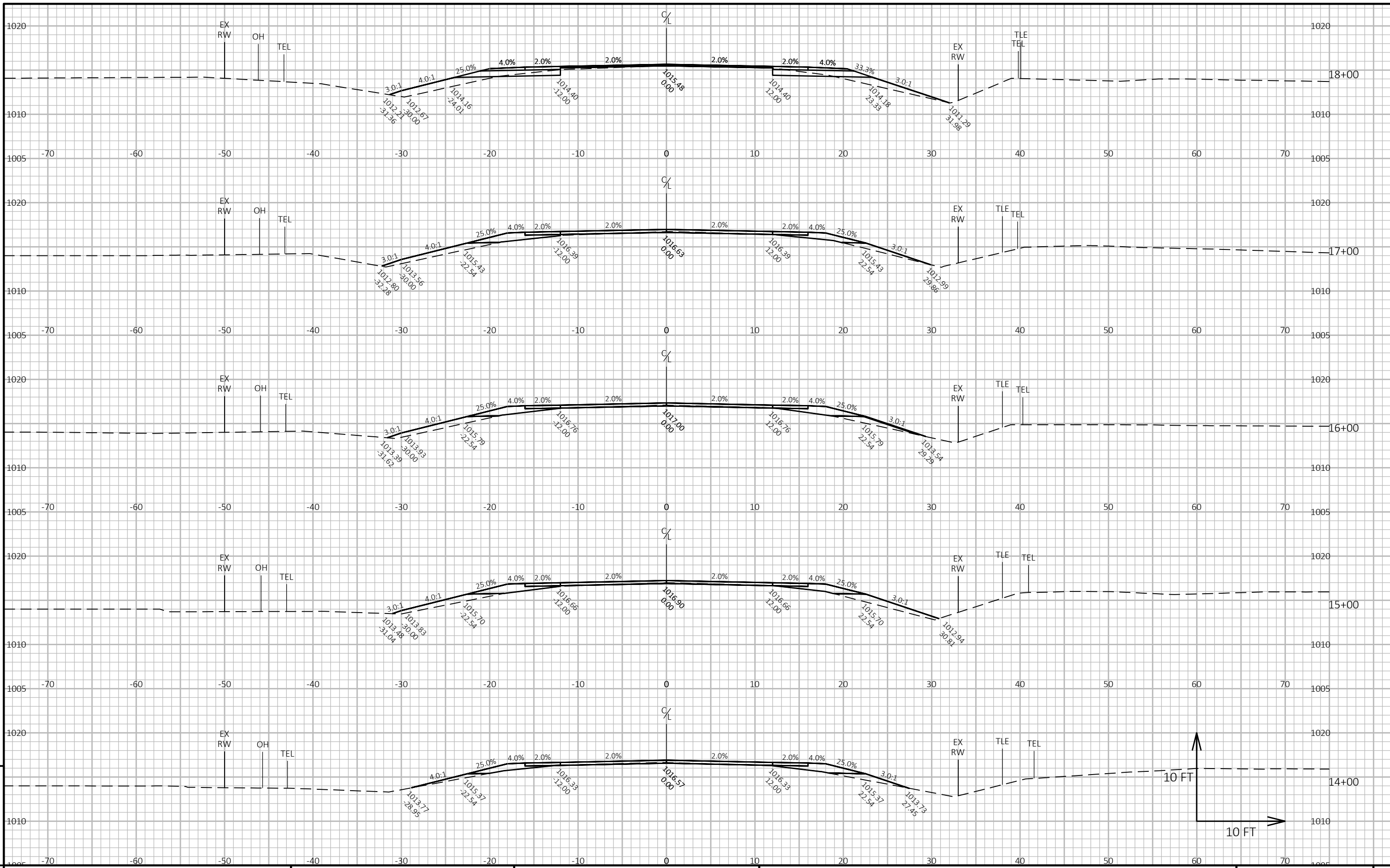
- (1) CUT INCLUDES SALVAGED/UNUSABLE PAVEMENT MATERIAL
- (2) SALVAGED/UNUSABLE PAVEMENT MATERIAL IS INCLUDED IN CUT.
- (3) FILL DOES NOT INCLUDE UNUSABLE PAVEMENT EXC VOLUME.
- (4) AVAILABLE MATERIAL = CUT - SALVAGED/UNUSABLE PAVEMENT MATERIAL
- (5) EXPANDED FILL FACTOR = 1.25
- DEPENDING ON SELECTIONS:
- (6) THE CUT AND FILL VALUES WERE OBTAINED FROM A SURFACE TO SURFACE VOLUMN COMPUTATION.
- (7) THE MASS ORDINATE + OR - QTY CALCULATED FOR THE STAGE.
- (8) MASS ORDINATE: [CUT - EXPANDED FILL] * (FILL FACTOR ADJUSTED)



PROJECT NO: 8941-05-71 HWY: CTH K COUNTY: ST CROIX CROSS SECTIONS: CTH K SHEET E

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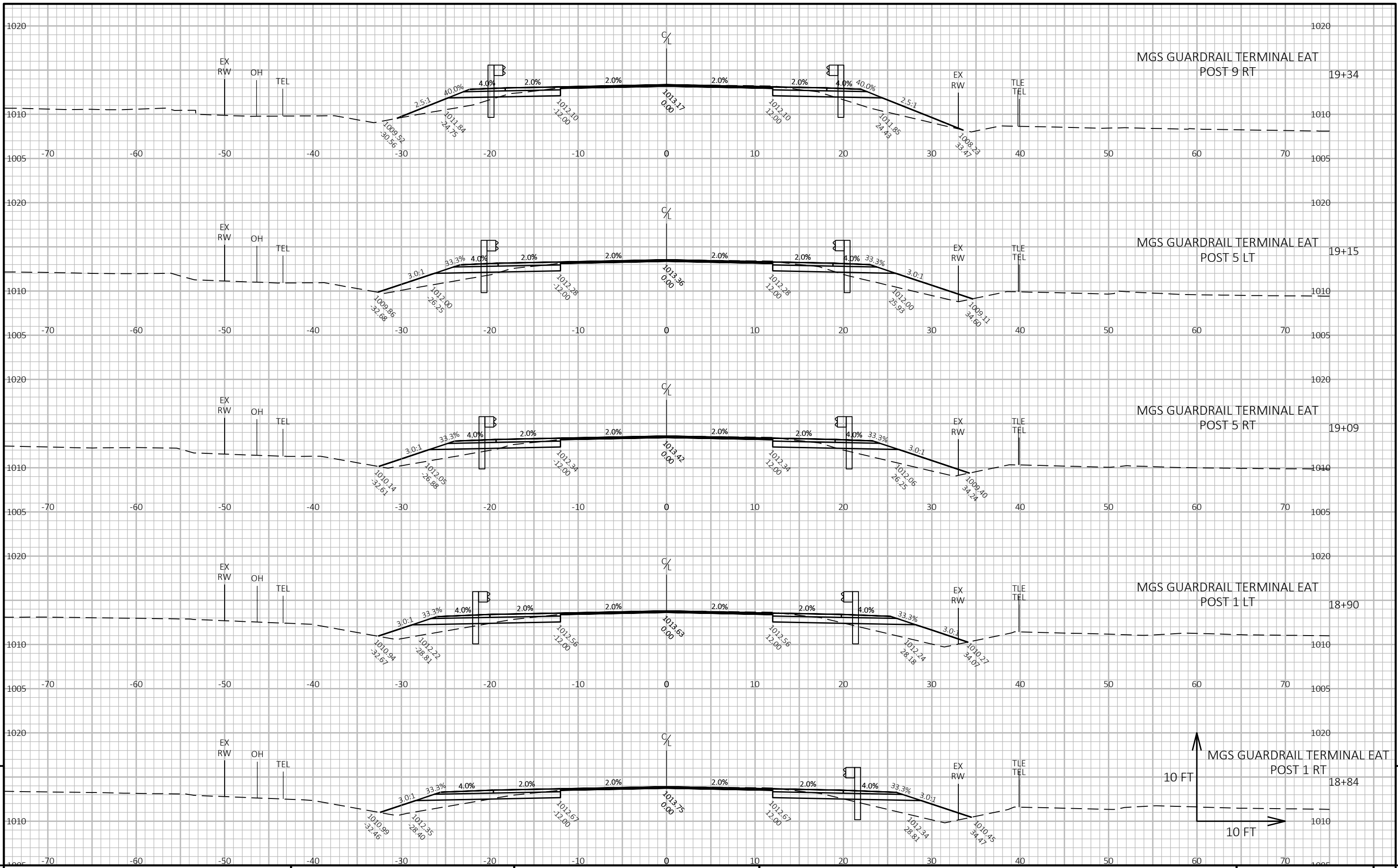
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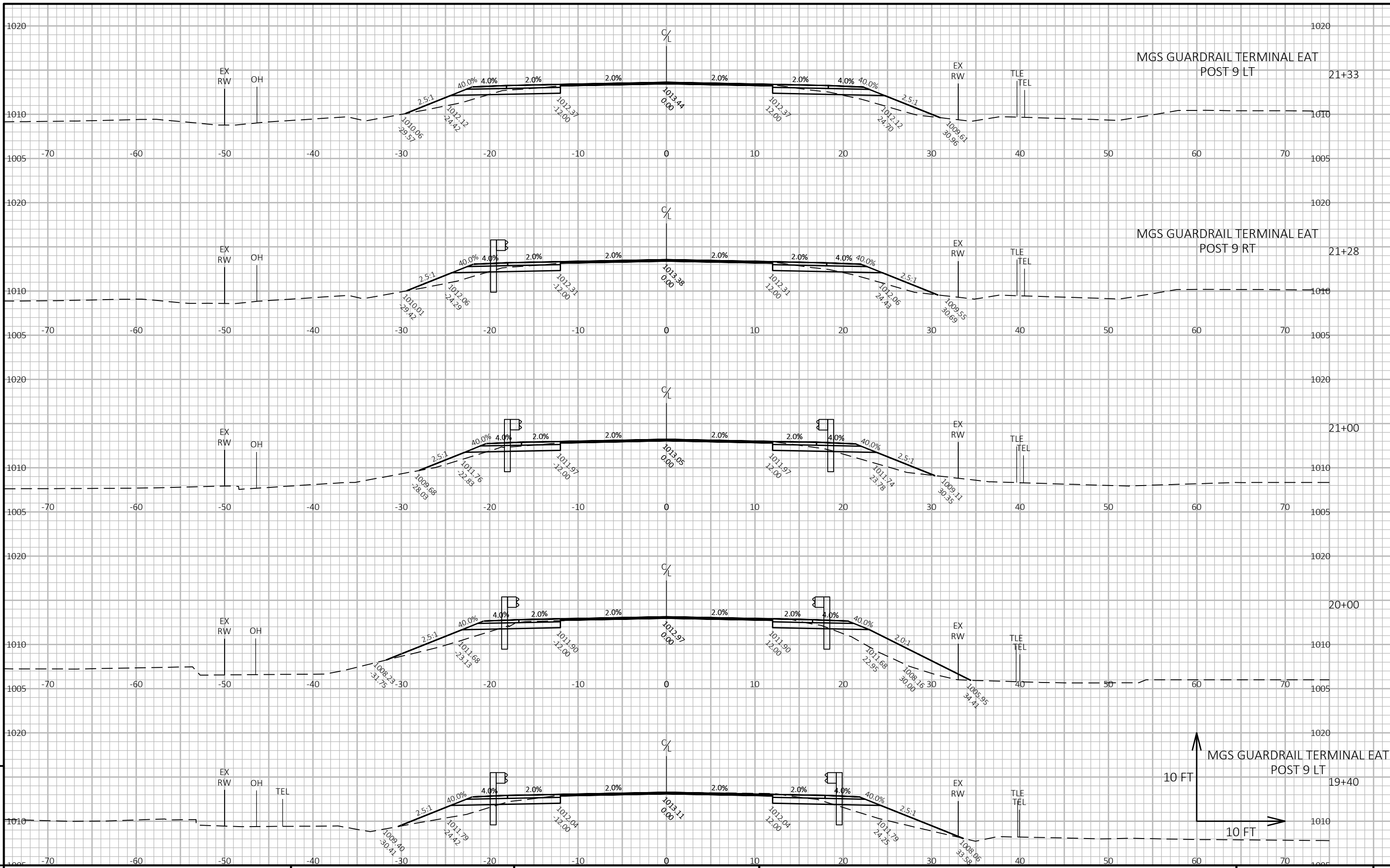
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LAYOUT NAME - 2

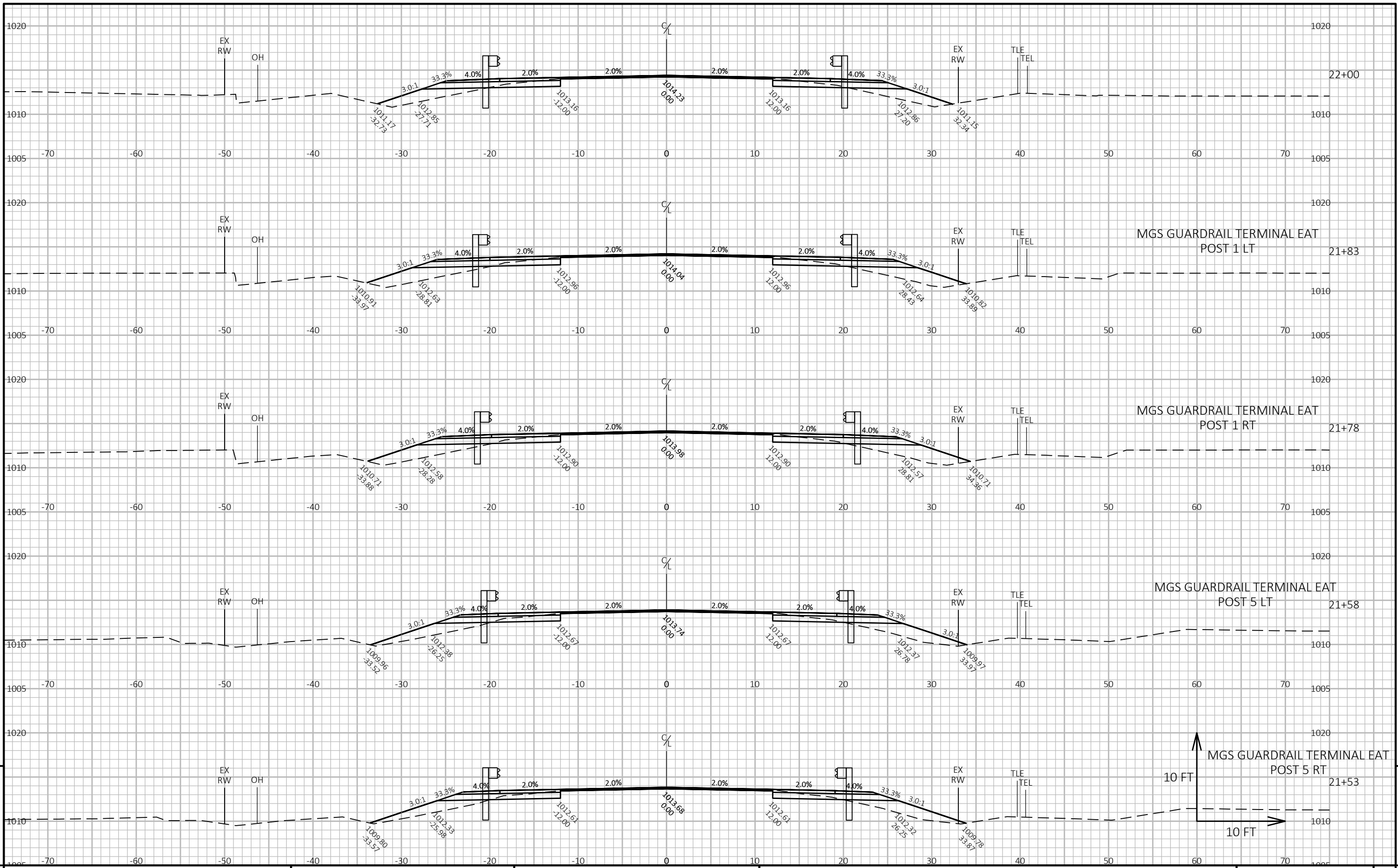


PROJECT NO: 8941-05-71 HWY: CTH K COUNTY: ST CROIX CROSS SECTIONS: CTH K SHEET 9



PROJECT NO: 8941-05-71 HWY: CTH K COUNTY: ST CROIX CROSS SECTIONS: CTH K SHEET E

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PROJECT NO: 8941-05-71

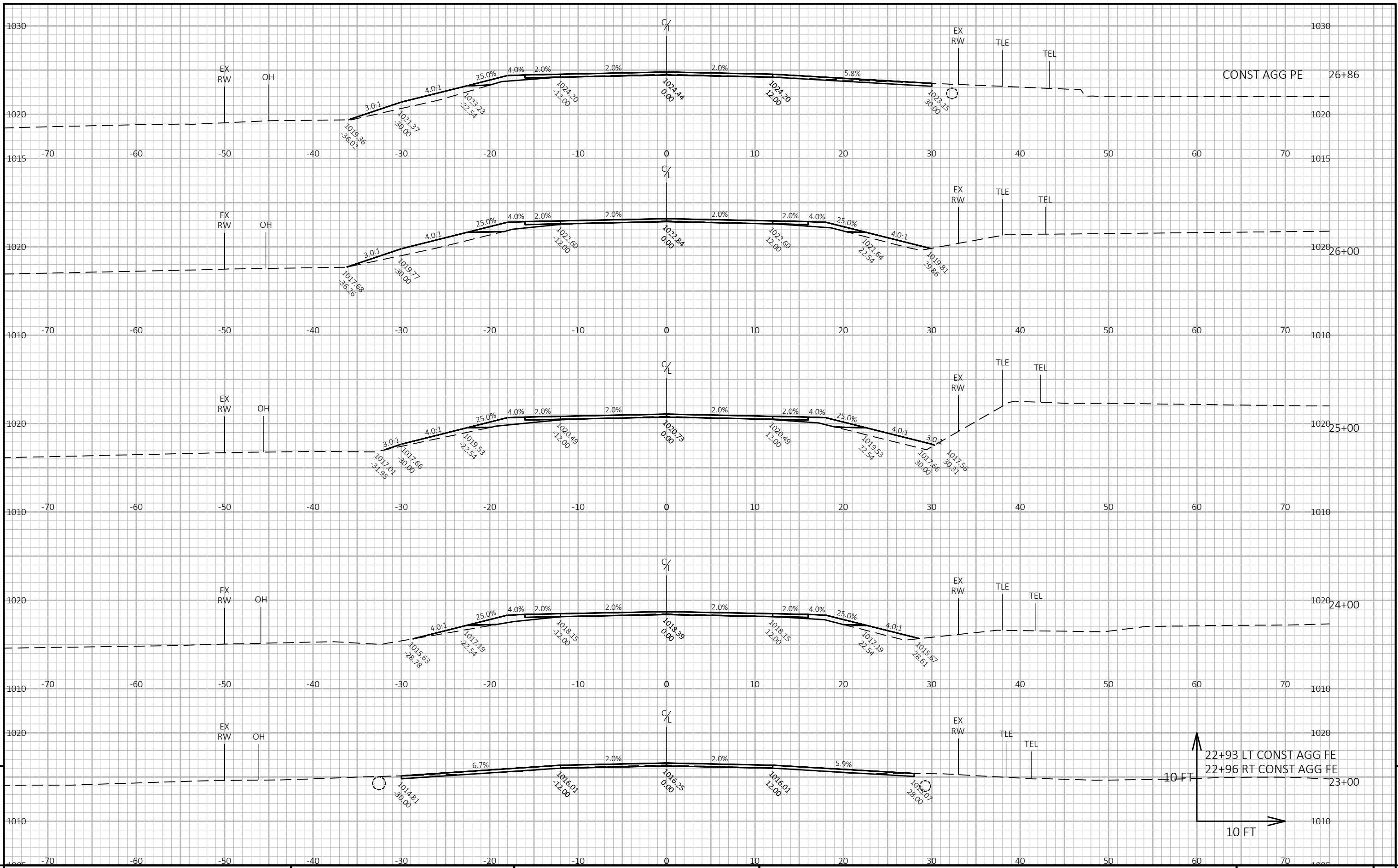
HWY: CTH K

COUNTY: ST CROIX

CROSS SECTIONS: CTH K

SHEET

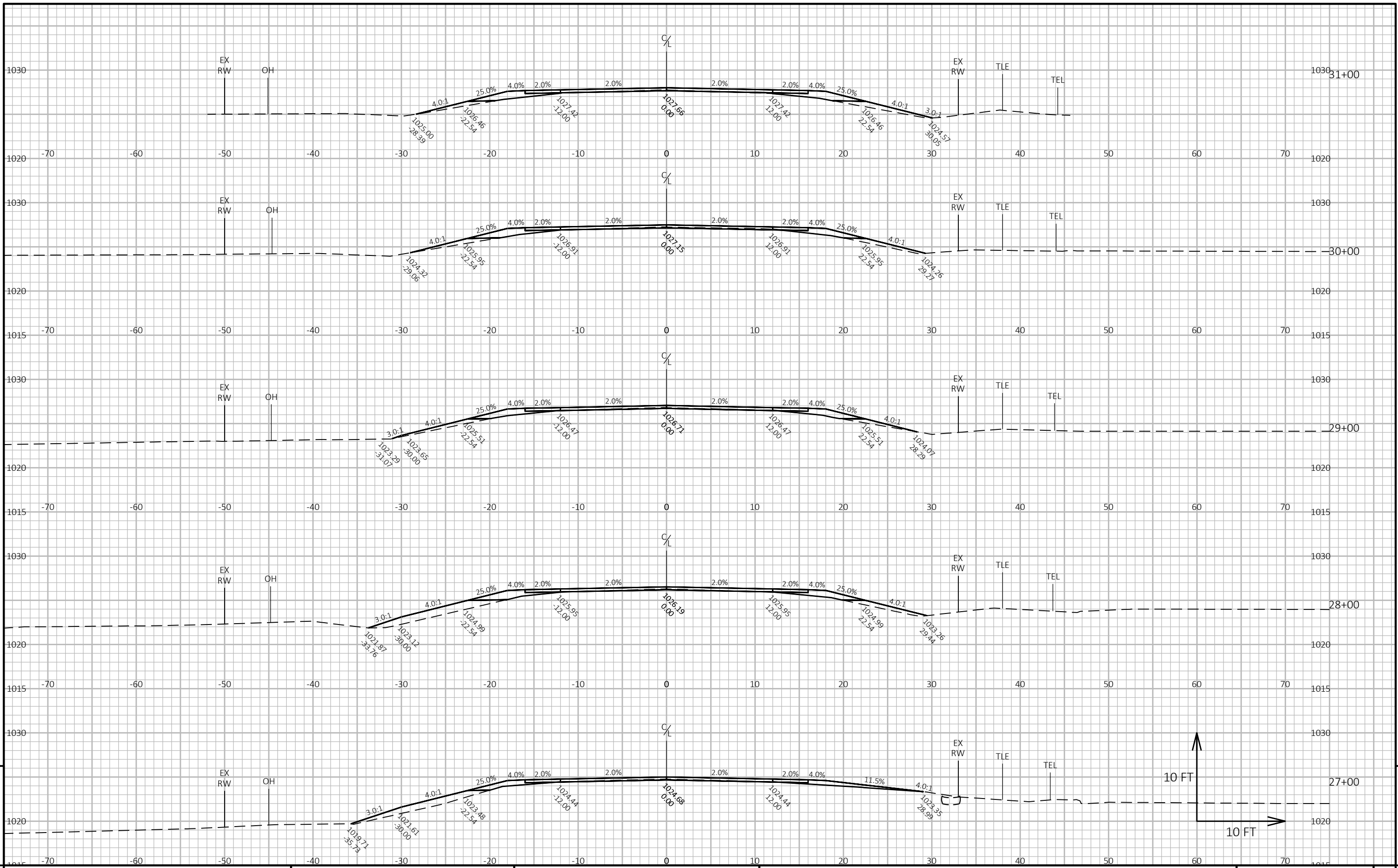
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PROJECT NO: 8941-05-71	HWY: CTH K	COUNTY: ST CROIX	CROSS SECTIONS: CTH K	SHEET	E
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PROJECT NO: 8941-05-71

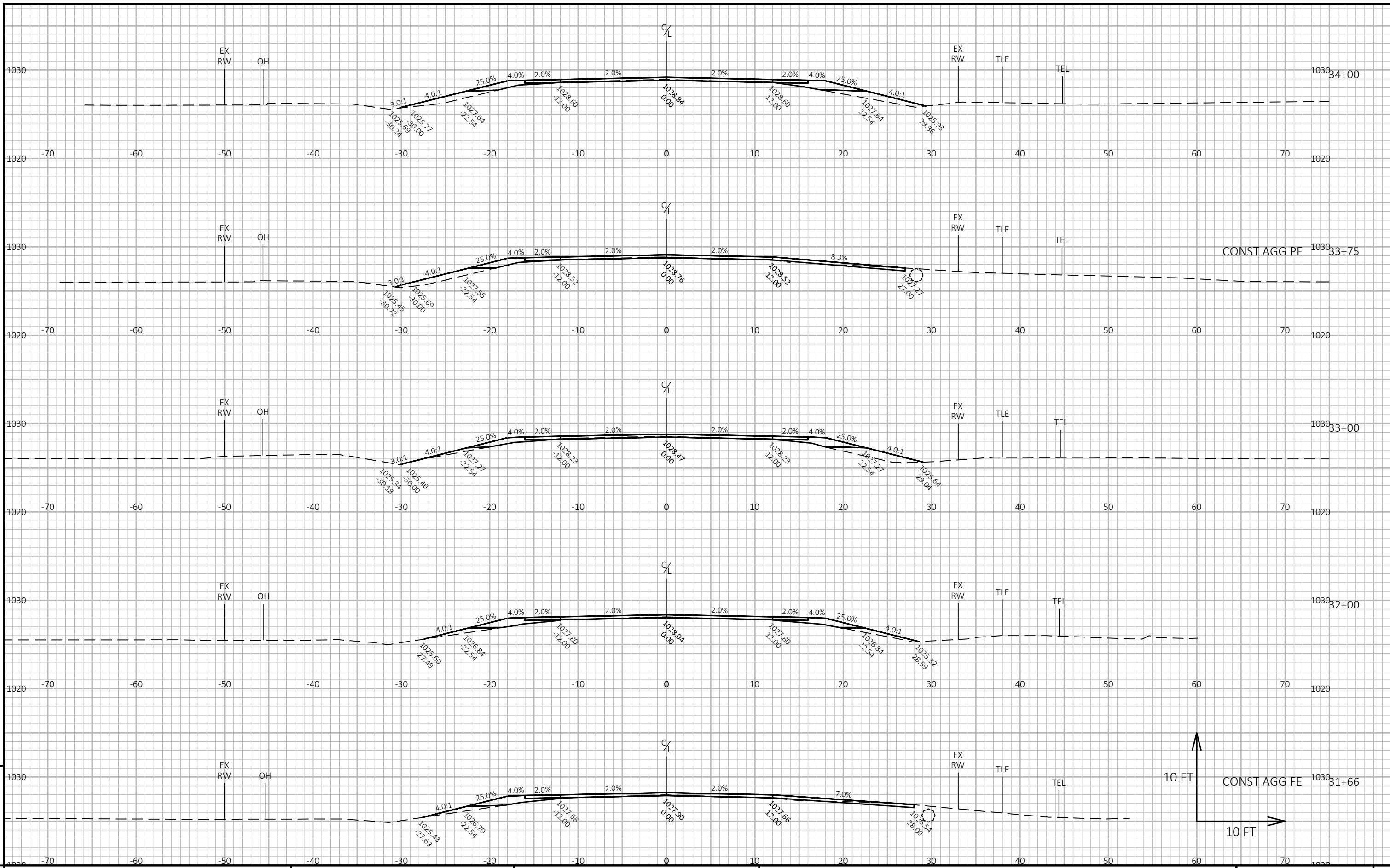
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COUNTY: ST CROIX

CROSS SECTIONS: CTH K

SHEET

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PROJECT NO: 8941-05-71

HWY: CTH K

COUNTY: ST CROIX

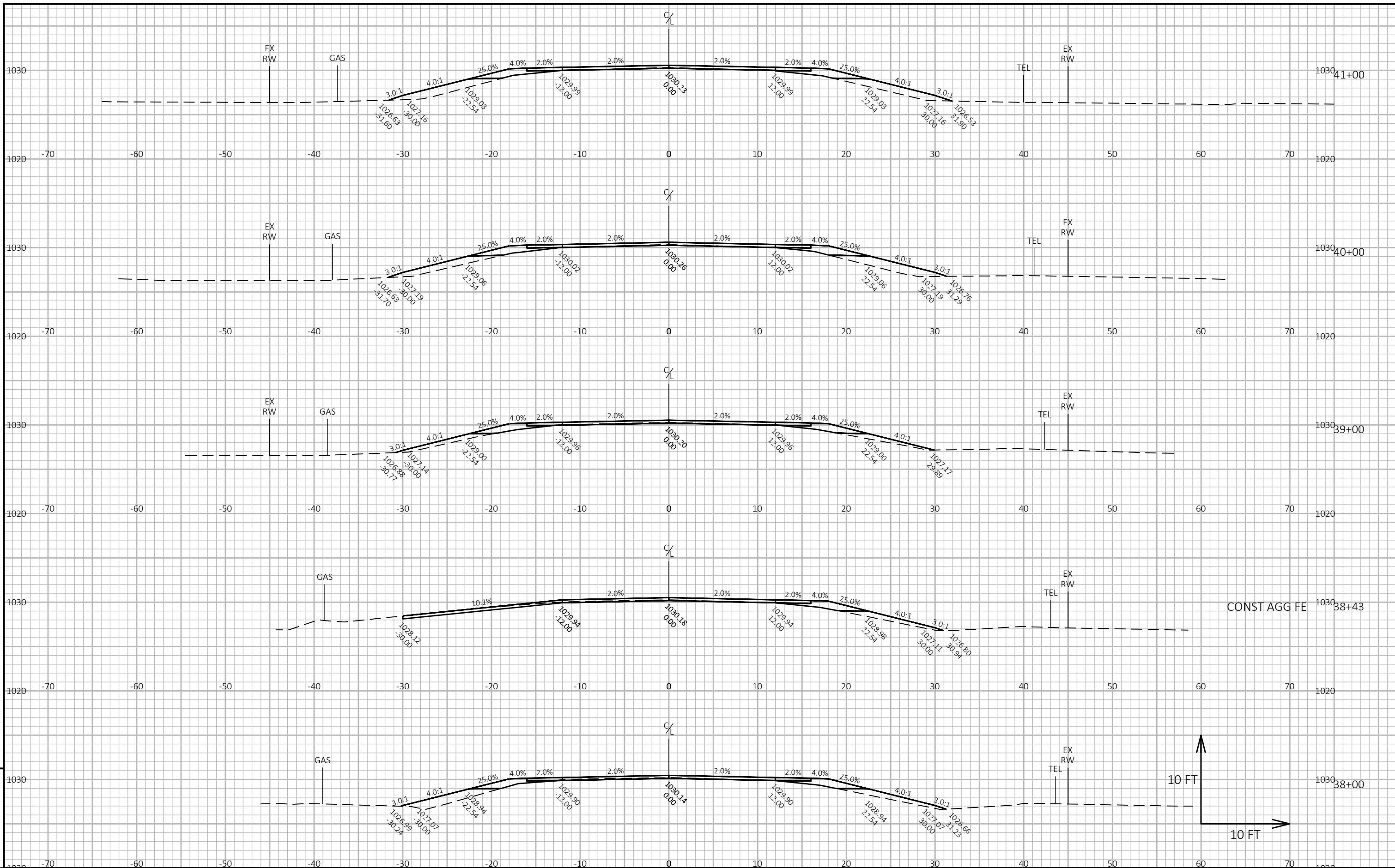
CROSS SECTIONS: CTH K

SHEET

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PROJECT NO: 8941-05-71 HWY: CTH K COUNTY: ST CROIX CROSS SECTIONS: CTH K SHEET 9



PROJECT NO: 8941-05-71

HWY: CTH K

COUNTY: ST CROIX

CROSS SECTIONS: CTH K

SHEET

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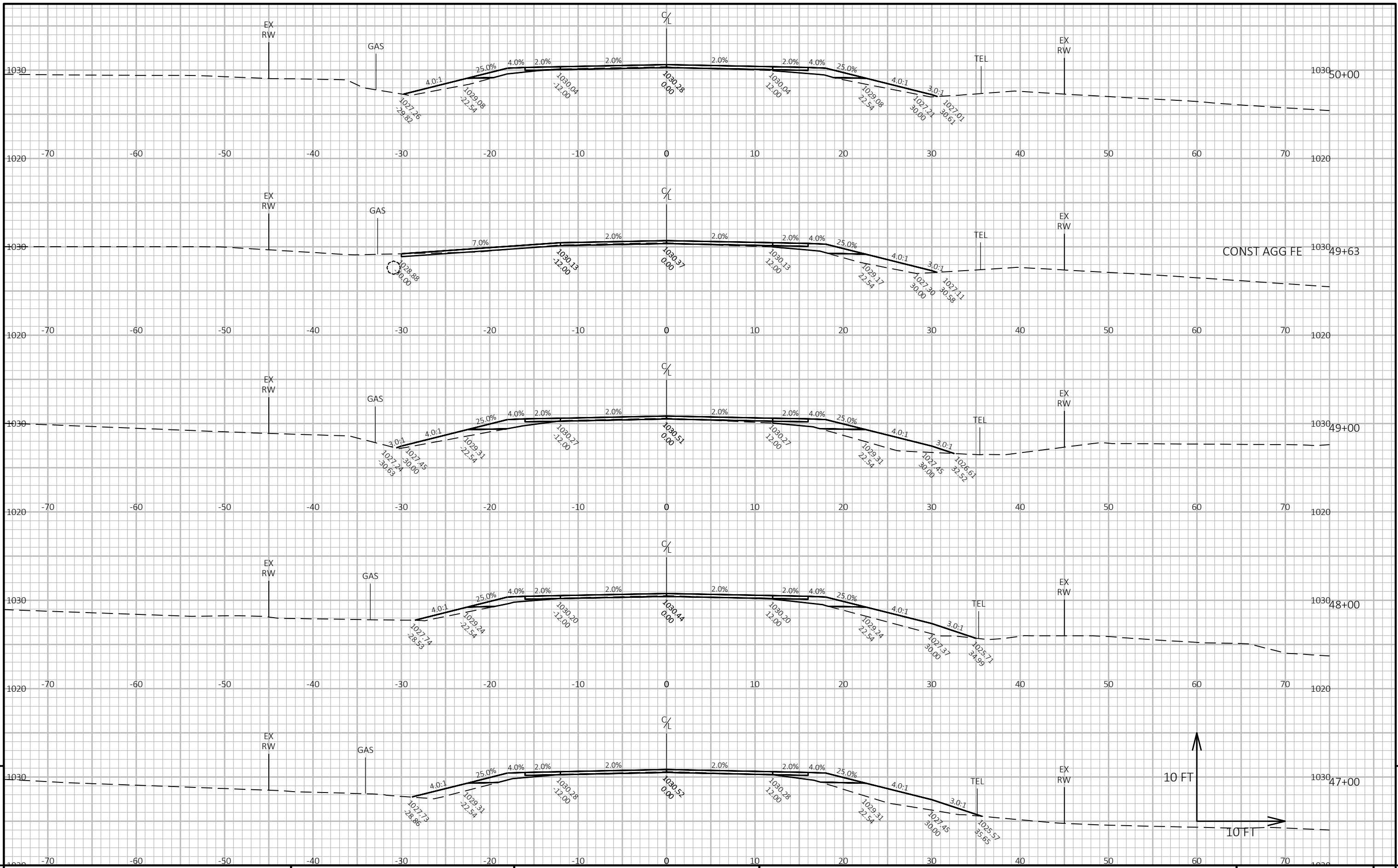
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PROJECT NO: 8941-05-71 HWY: CTH K COUNTY: ST CROIX CROSS SECTIONS: CTH K SHEET E

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LAYOUT NAME - 11



PROJECT NO: 8941-05-71

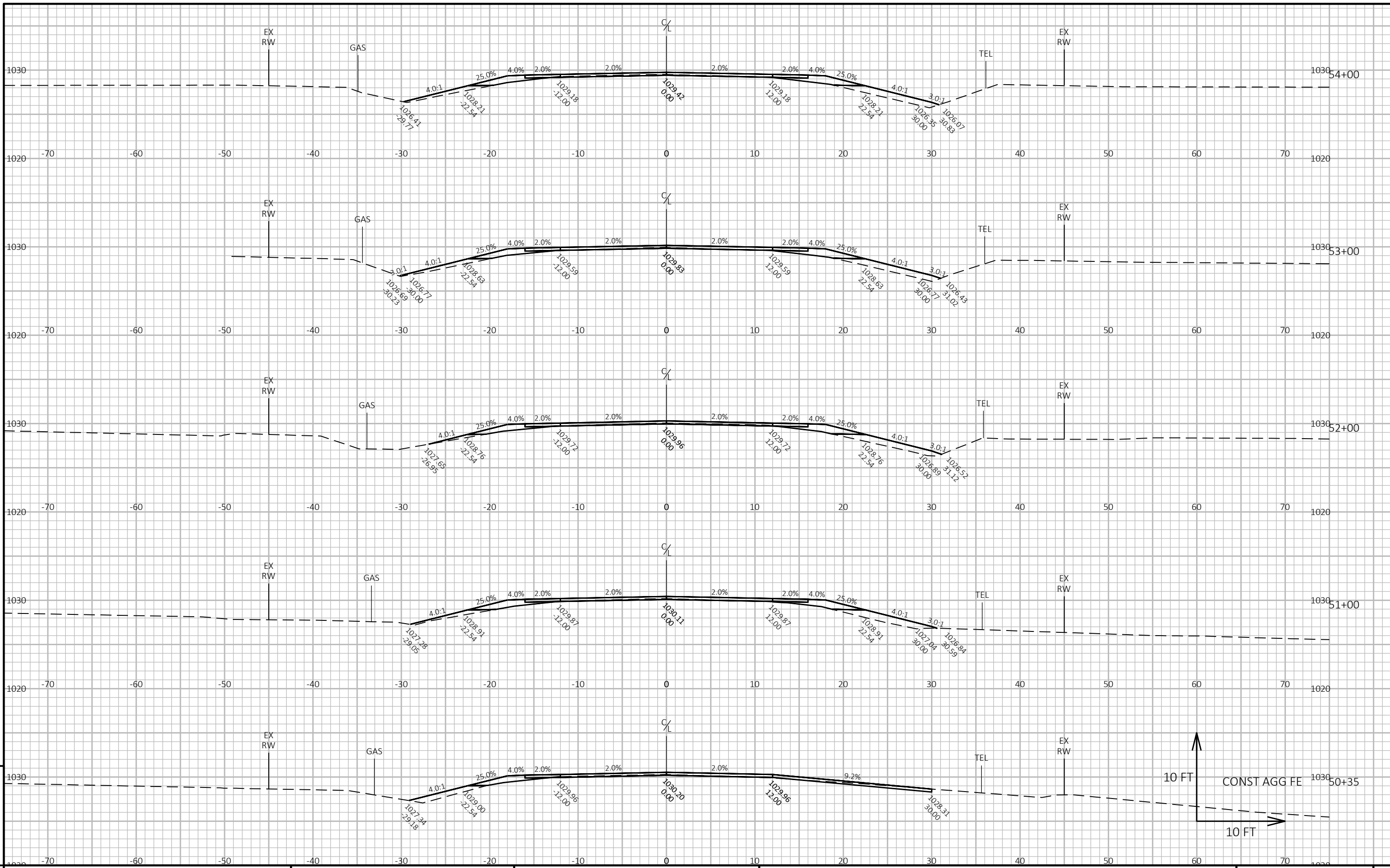
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COUNTY: ST CROIX

CROSS SECTIONS: CTH K

SHEET

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PROJECT NO: 8941-05-71

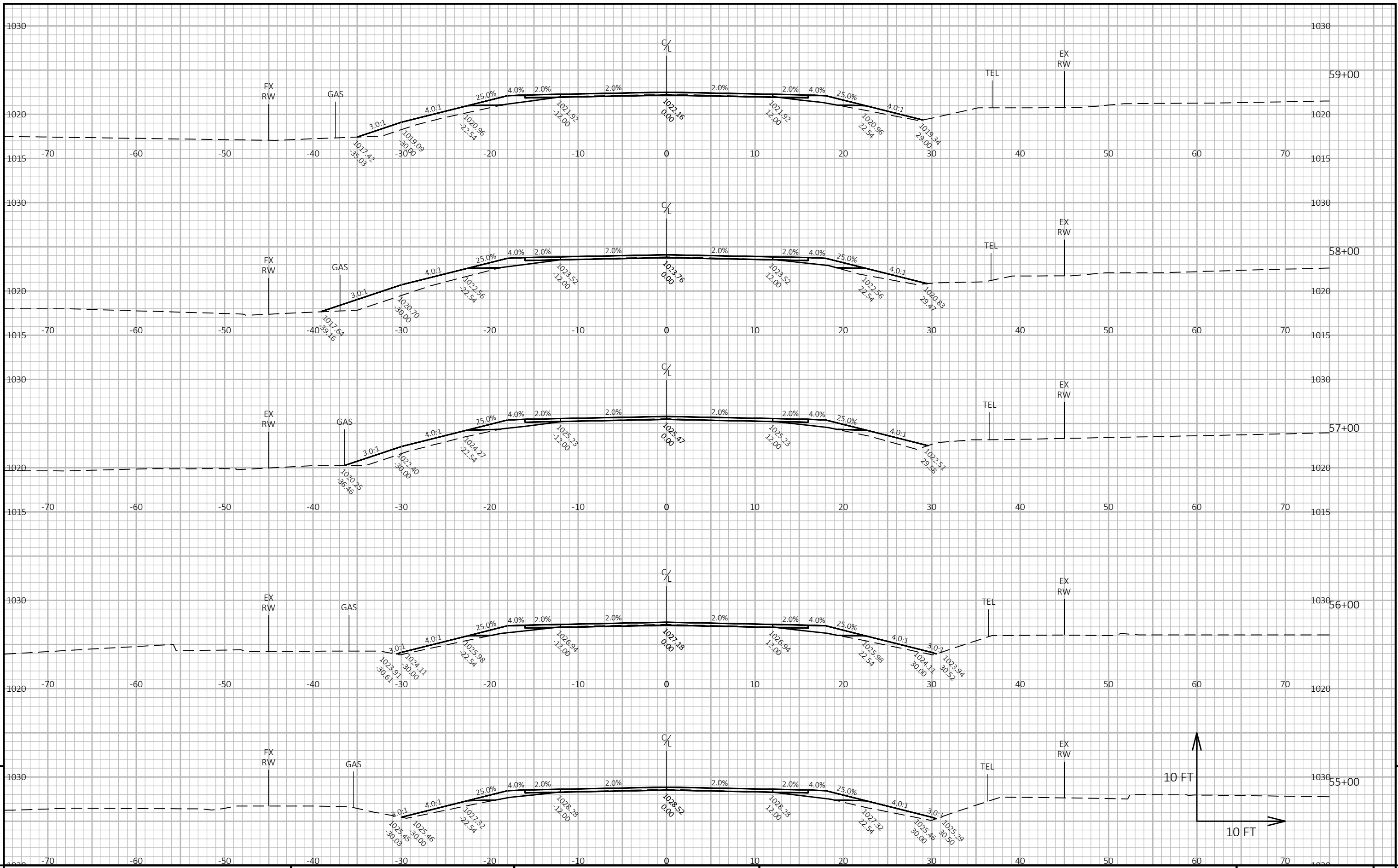
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COUNTY: ST CROIX

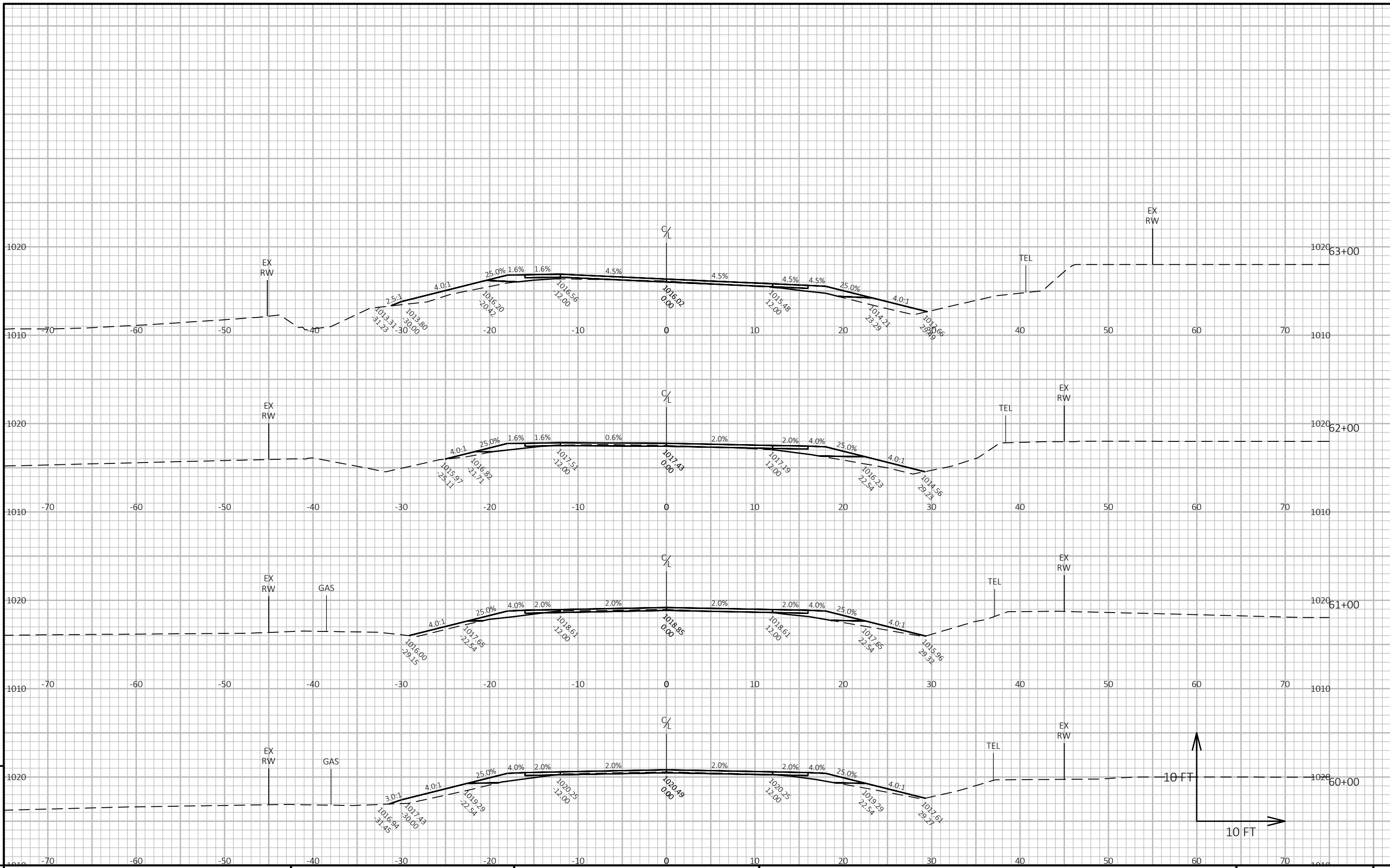
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SHEET

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PROJECT NO: 8941-05-71	HWY: CTH K	COUNTY: ST CROIX	CROSS SECTIONS: CTH K	SHEET	E
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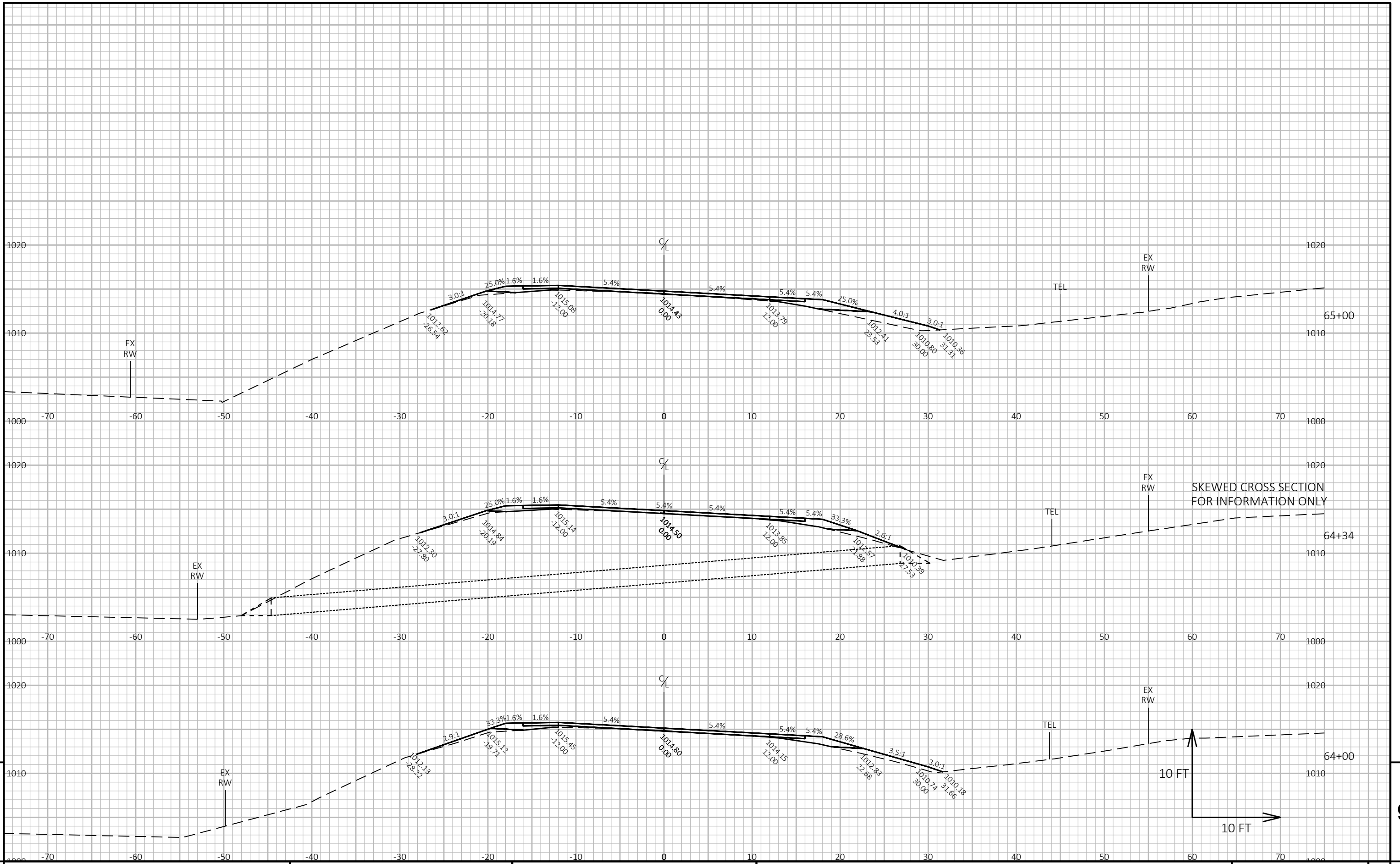
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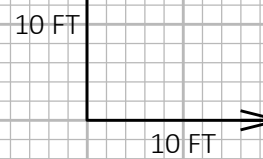
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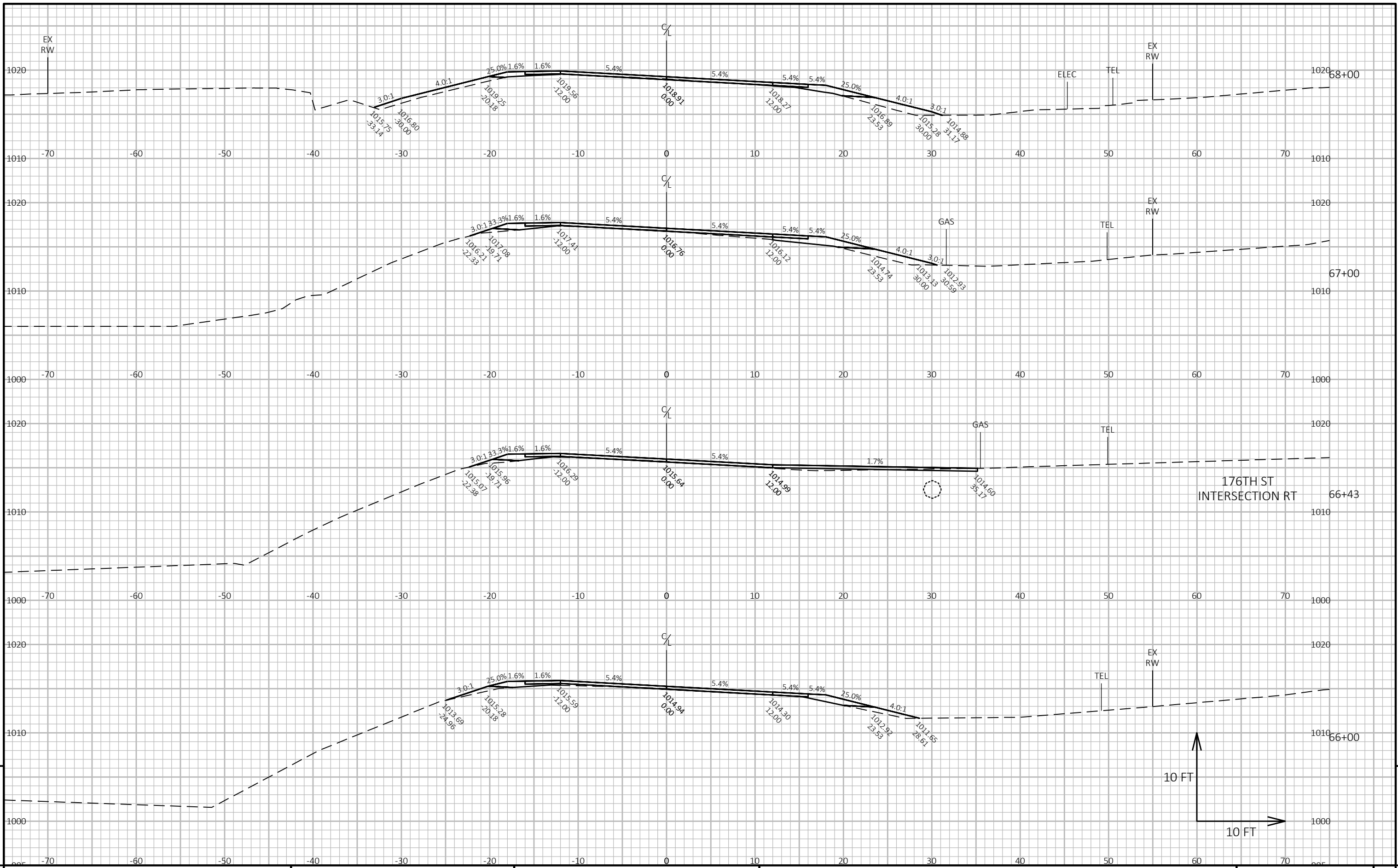
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SKewed CROSS SECTION FOR INFORMATION ONLY





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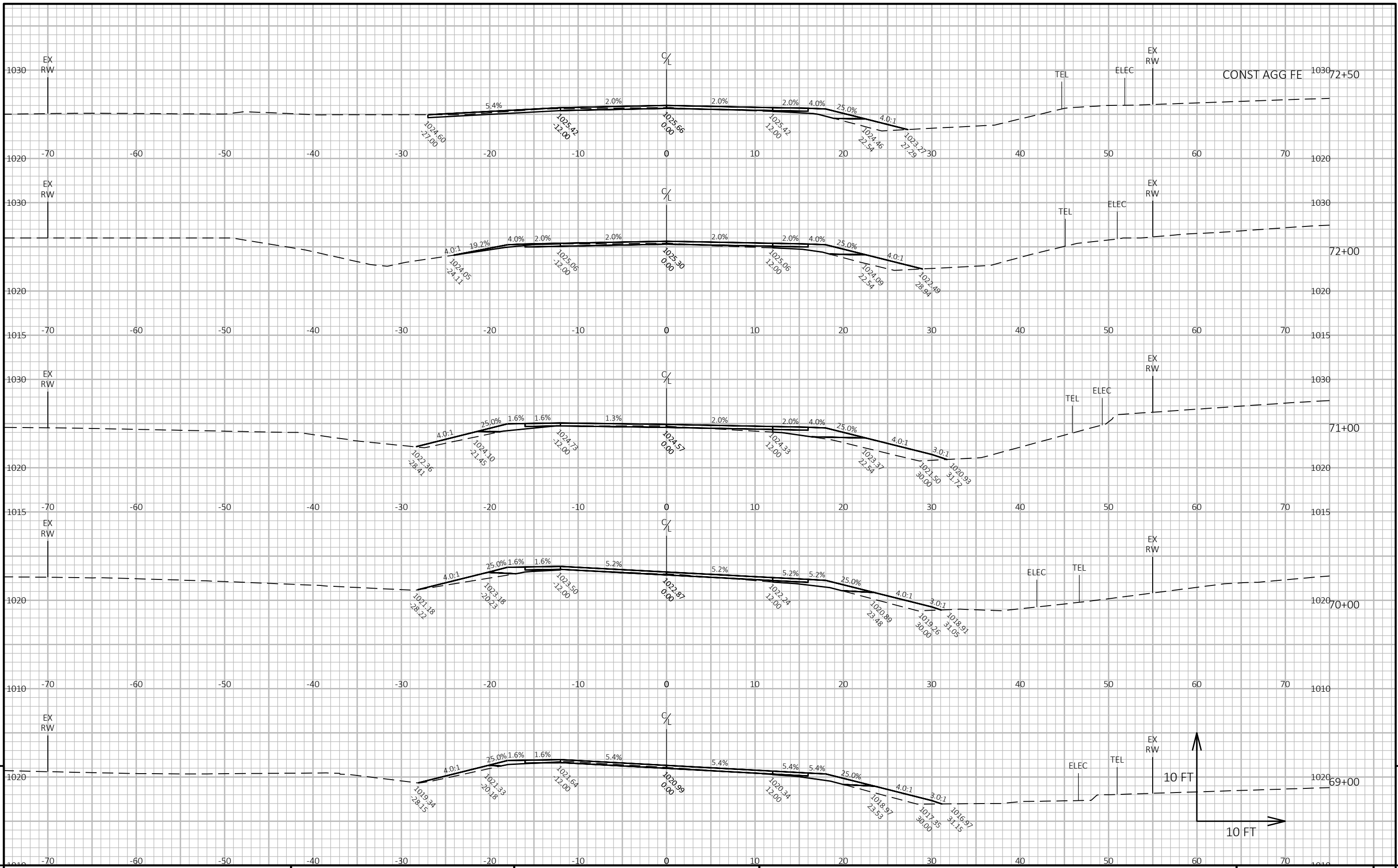
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COUNTY: ST CROIX

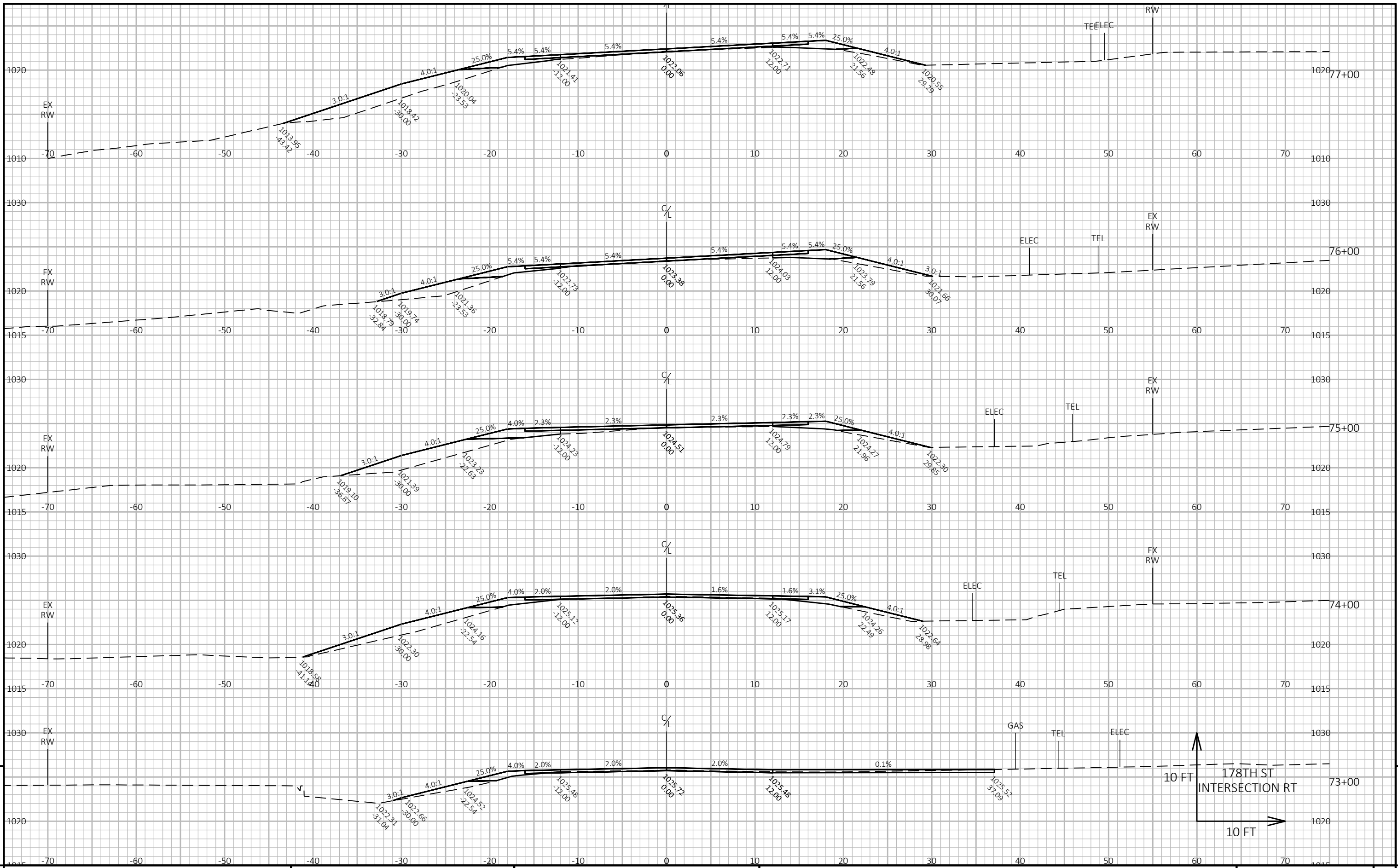
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PROJECT NO: 8941-05-71 HWY: CTH K COUNTY: ST CROIX CROSS SECTIONS: CTH K SHEET E



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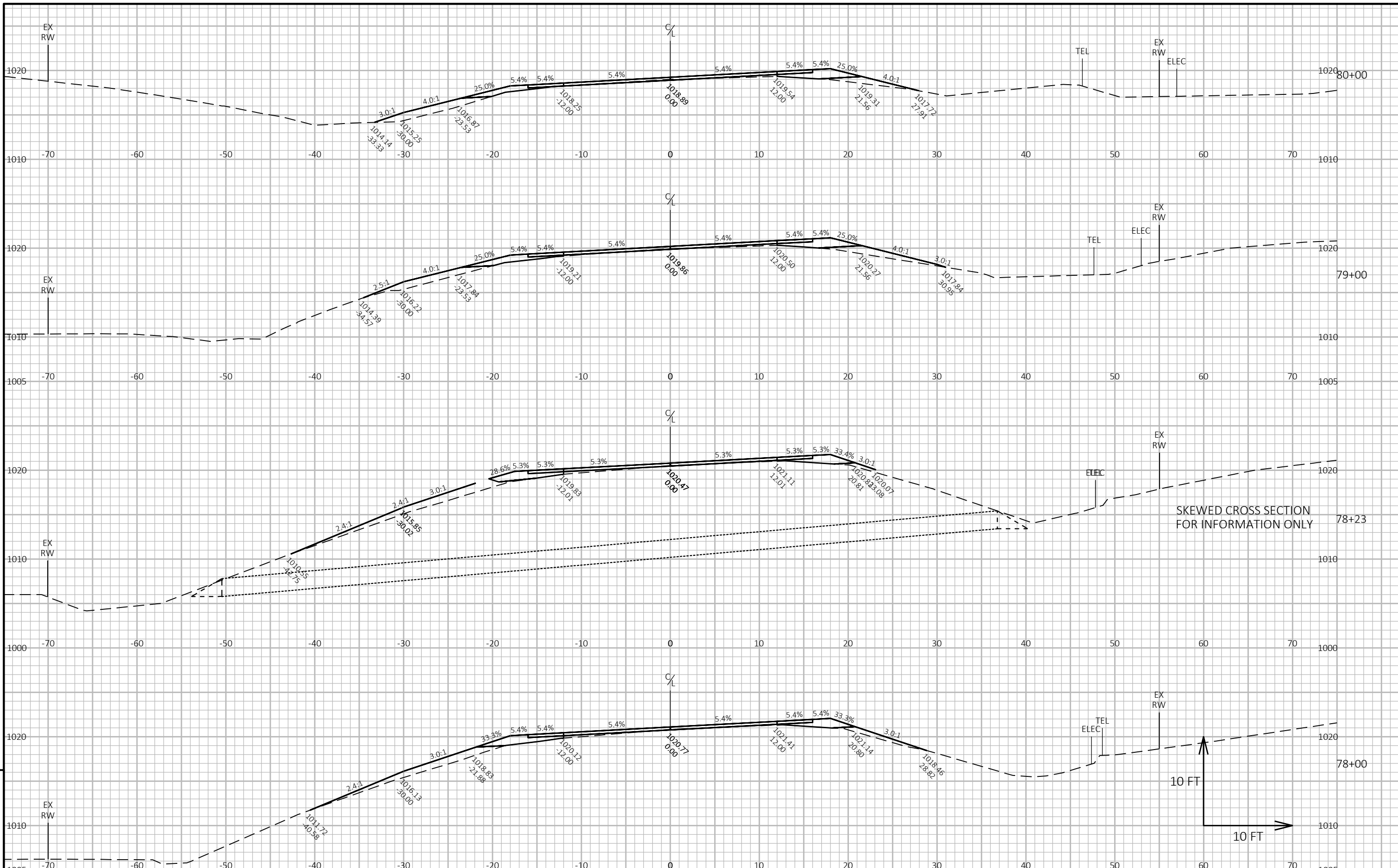
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COUNTY: ST CROIX

CROSS SECTIONS: CTH K

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PROJECT NO: 8941-05-71

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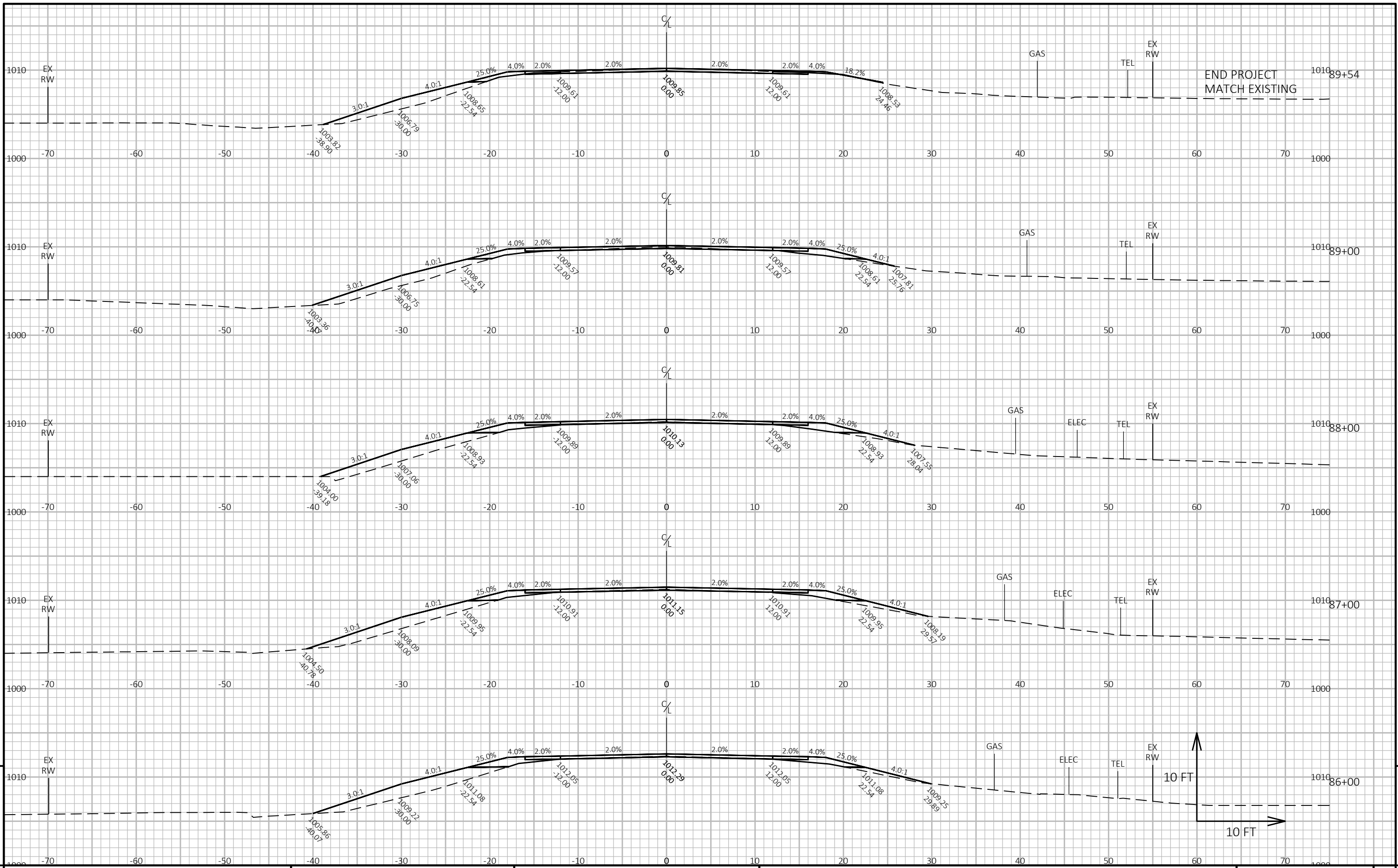
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SHEET

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PROJECT NO: 8941-05-71 HWY: CTH K COUNTY: ST CROIX CROSS SECTIONS: CTH K SHEET E



PROJECT NO: 8941-05-71

HWY: CTH K

COUNTY: ST CROIX

CROSS SECTIONS: CTH K

SHEET

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Notes



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