

ORDER OF SHEETS

Section No.	1	Title
Section No.	2	Typical Sections and Details
Section No.	3	Estimate of Quantities
Section No.	3	Miscellaneous Quantities
Section No.	4	Right of Way Plat
Section No.	5	Plan and Profile
Section No.	6	Standard Detail Drawings
Section No.	7	Sign Plates
Section No.	8	Structure Plans
Section No.	9	Computer Earthwork Data
Section No.	9	Cross Sections

TOTAL SHEETS = 36



33

DESIGN DESIGNATION

A.A.D.T.	2D23	=	N/A
A.A.D.T.	2D43	=	40
D.H.V.		=	10
D.D.		=	50/50
T.		=	13% ASSUMED
DESIGN SPEED		=	35 MPH
ESALS		=	36,000

CONVENTIONAL SYMBOLS

PLAN

CORPORATE LIMITS	
PROPERTY LINE	
LOT LINE	
LIMITED HIGHWAY EASEMENT	
EXISTING RIGHT OF WAY	
PROPOSED OR NEW R/W LINE	
SLOPE INTERCEPT	
REFERENCE LINE	
EXISTING CULVERT	
PROPOSED CULVERT (Box or Pipe)	
COMBUSTIBLE FLUIDS	
MARSH AREA	
WOODED OR SHRUB AREA	

PROFILE

GRADE LINE	
ORIGINAL GROUND	
MARSH OR ROCK PROFILE (To be noted as such)	
SPECIAL DITCH	
GRADE ELEVATION	
CULVERT (Profile View)	
UTILITIES	
ELECTRIC	
FIBER OPTIC	
GAS	
SANITARY SEWER	
STORM SEWER	
TELEPHONE	
WATER	
UTILITY PEDESTAL	
POWER POLE	
TELEPHONE POLE	

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

T BEAVER, KINGTON ROAD

NELSON CREEK BRIDGE P-10-0902

LOCAL STREET CLARK COUNTY

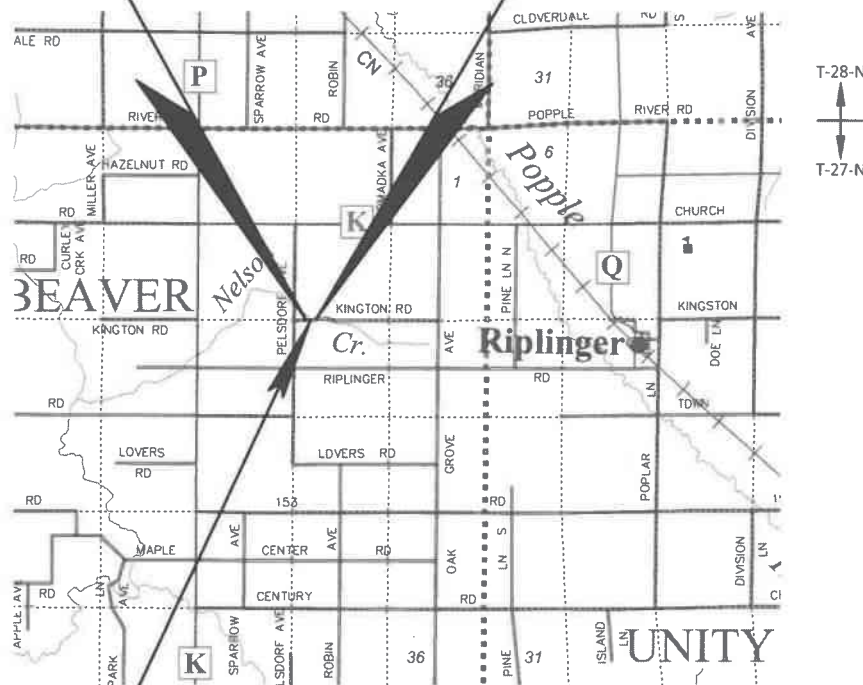
STATE PROJECT NUMBER
7841-00-03

BEGIN PROJECT 7841-00-03

STA. 9+25.00
Y = 447,824.010
X = 171,138.912

END PROJECT 7841-00-03

STA. 10+75.00
Y = 447,830.464
X = 717,288.765



STRUCTURE B-10-255
STA. 10+00.00



TOTAL NET LENGTH OF CENTERLINE = 0.028 MI

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS), CLARK COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES.

ELEVATIONS ARE REFERENCED TO NAVD 88 (2112). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 12A

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
7841-00-03	WISC 2024350	1

ACCEPTED FOR

TOWN OF BEAVER

1-21-24 *Mohr Mahm*
Date (Signature and Title of Official)

ORIGINAL PLANS PREPARED BY

Mead & Hunt



DATE: 1/11/2024 *Jay P. Wheaton*
(Professional Engineer Signature)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY	
Surveyor	MEAD & HUNT
Designer	MEAD & HUNT
Project Manager	JEFFREY OLSON
Regional Examiner	TOU YANG
Regional Supervisor	JEFFREY OLSON

APPROVED FOR THE DEPARTMENT Jeffrey G. Olson
DATE: 2024.01.25 13:20:19 -06'07'
(Signature)

E

GENERAL NOTES

THE LOCATION OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS IS APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

NO TREES OR SHRUBS ARE TO BE REMOVED WITHOUT THE APPROVAL OF THE ENGINEER.

THE EXACT LOCATION OF THE EROSION CONTROL DEVICES SHALL BE DETERMINED IN THE FIELD. SILT FENCE IS TO BE PLACED AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER, AND IN PLACE PRIOR TO BRIDGE REMOVAL.

DISTURBED AREAS WITHIN THE RIGHT-OF-WAY, EXCEPT THE AREAS WITHIN THE FINISHED SHOULDER POINTS, ARE TO BE 4-INCH SALVAGED TOPSOIL, FERTILIZED, SEEDED AND EROSION MAT URBAN CLASS I TYPE B.

BEARINGS SHOWN ON THE PLANS ARE GROUND BEARINGS TO THE NEAREST SECOND.

TEMPORARY STORAGE OF ANY EXCAVATED MATERIALS WILL NOT BE PERMITTED IN THE WETLANDS.

WETLANDS ARE PRESENT WITHIN THE PROJECT LIMITS. DO NOT OPERATE EQUIPMENT OUTSIDE THE SLOPE INTERCEPTS.

EXISTING STRUCTURE P-10-902 IS NOT OPEN TO TRAFFIC. EXISTING TRAFFIC CONTROL TO BE REMOVED BY TOWNSHIP AFTER PROPOSED TRAFFIC CONTROL HAS BEEN INSTALLED.

TOWN OF BEAVER TO COMPLETE TREE CLEARING BY MARCH 31, 2024.

ORDER OF SECTION 2 SHEETS

TYPICAL SECTIONS
EROSION CONTROL
TRAFFIC CONTROL

STANDARD ABBREVIATIONS

ADT	AVERAGE DAILY TRAFFIC	M/L	MAINLINE
AGG	AGGREGATE	NO	NUMBER
ASPH	ASPHALTIC	PE	PRIVATE ENTRANCE
BM	BENCH MARK	PI	POINT OF INTERSECTION
BOC	BACK OF CURB	PL	PROPERTY LINE
C&G	CURB AND GUTTER	PP	POWER POLE
CE	COMMERCIAL ENTRANCE	QTY	QUANTITY
CL	CENTERLINE	RHF	RIGHT-HAND FORWARD
COR	CORNER	RT	RIGHT
CWT	HUNDREDWEIGHT	R/L	REFERENCE LINE
CY	CUBIC YARD	R/W	RIGHT-OF-WAY
DHV	DESIGN HOURLY VOLUME	SF	SQUARE FOOT
DWY	DRIVEWAY	SHLDR	SHOULDER
EL	ELEVATION	SS	STORM SEWER
EX	EXISTING	STA	STATION
EXC	EXCAVATION	SY	SQUARE YARD
FT	FOOT	T	TRUCKS (PERCENT OF)
FTG	FOOTING	TEL	TELEPHONE
HYD	HYDRANT	TLE	TEMPORARY LIMITED EASEMENT
INV	INVERT	TYP	TYPICAL
LB	POUND	UG	UNDERGROUND CABLE
LF	LINEAR FOOT	VAR	VARIABLE
LHF	LEFT-HAND FORWARD	VC	VERTICAL CURVE
LS	LUMP SUM	VPC	VERTICAL POINT OF CURVE
LT	LEFT	VPI	VERTICAL POINT OF INTERSECTION
Mgal	MEGAGALLON	VPT	VERTICAL POINT OF TANGENCY

CONTACTS

CLARK COUNTY HIGHWAY DEPT.
MR. BRIAN DUELL
COUNTY COMMISSIONER
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EMAIL: BRIAN.DUELL@CO.CLARK.WI.US

TOWN OF BEAVER
MR. MITCH MALM
TOWN OF BEAVER CHAIRMAN
W3742 CAPITAL ROAD
LOYAL, WI 54446
PHONE: (715) 897-4653
EMAIL: BEAVERTOWNHALL@GMAIL.COM

DNR CONTACT
MR. BRAD BETTHAUSER
BLACK RIVER FALLS DNR SERVICE CENTER
910 HWY 54 E
BLACK RIVER FALLS, WI 54615
PHONE: (715) 213-9064
EMAIL: BRADLEY.BETTHAUSER@WISCONSIN.GOV

CONSULTANT CONTACT
MEAD & HUNT, INC.
750 NORTH THIRD STREET
LA CROSSE, WI 54601
ATTN: MR. JAY P. WHEATON, P.E.
PHONE: (608) 784-6040
MOBILE: (608) 386-0212
EMAIL: JAY.WHEATON@MEADHUNT.COM

UTILITIES

**** FRONTIER COMMUNICATIONS**
MR. RUSSELL RYAN
118 DIVISION STREET
PLYMOUTH, WI 53073
PHONE: (920) 583-7397
EMAIL: RUSSELL.W.RYAN@FTR.COM

CLARK ELECRCIC COOPERATIVE
MR. KENT L. WEIGEL
1209 W DALL-BERG RD
GREENWOOD, WI 54437
PHONE: (715) 267-7955
CELL: (715) 207-8883
EMAIL: KWEIGEL@CECOOP.COM

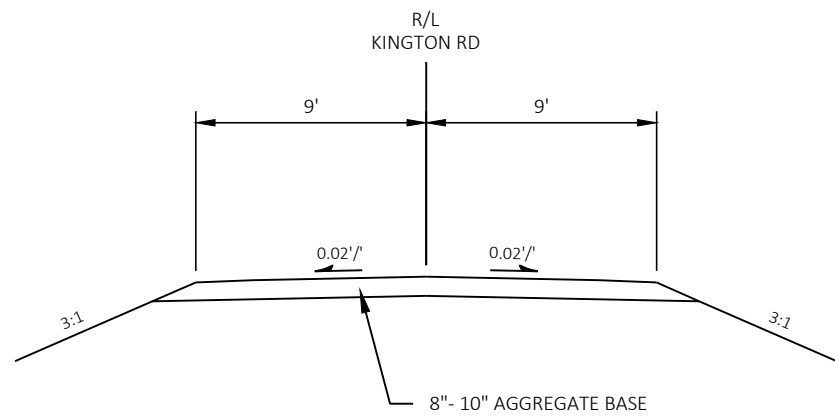
**** THESE ARE MEMBERS OF DIGGERS HOTLINE.**

RUNOFF COEFFICIENT TABLE

	HYDROLOGIC SOIL GROUP											
	A			B			C			D		
	SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)		
LAND USE:	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER
ROW CROPS	.08 .22	.16 .30	.22 .38	.12 .26	.20 .34	.27 .44	.15 .30	.24 .37	.33 .50	.19 .34	.28 .41	.38 .56
MEDIAN STRIP-TURF	.19 .24	.20 .26	.24 .30	.19 .25	.22 .28	.26 .33	.20 .26	.23 .30	.30 .37	.20 .27	.25 .32	.30 .40
SIDE SLOPE-TURF			.25 .32			.27 .34			.28 .36			.30 .38
PAVEMENT:												
ASPHALT	.70 - .95											
CONCRETE	.80 - .95											
BRICK	.70 - .80											
DRIVES, WALKS	.75 - .85											
ROOFS	.75 - .95											
GRAVEL ROADS, SHOULDERS	.40 - .60											

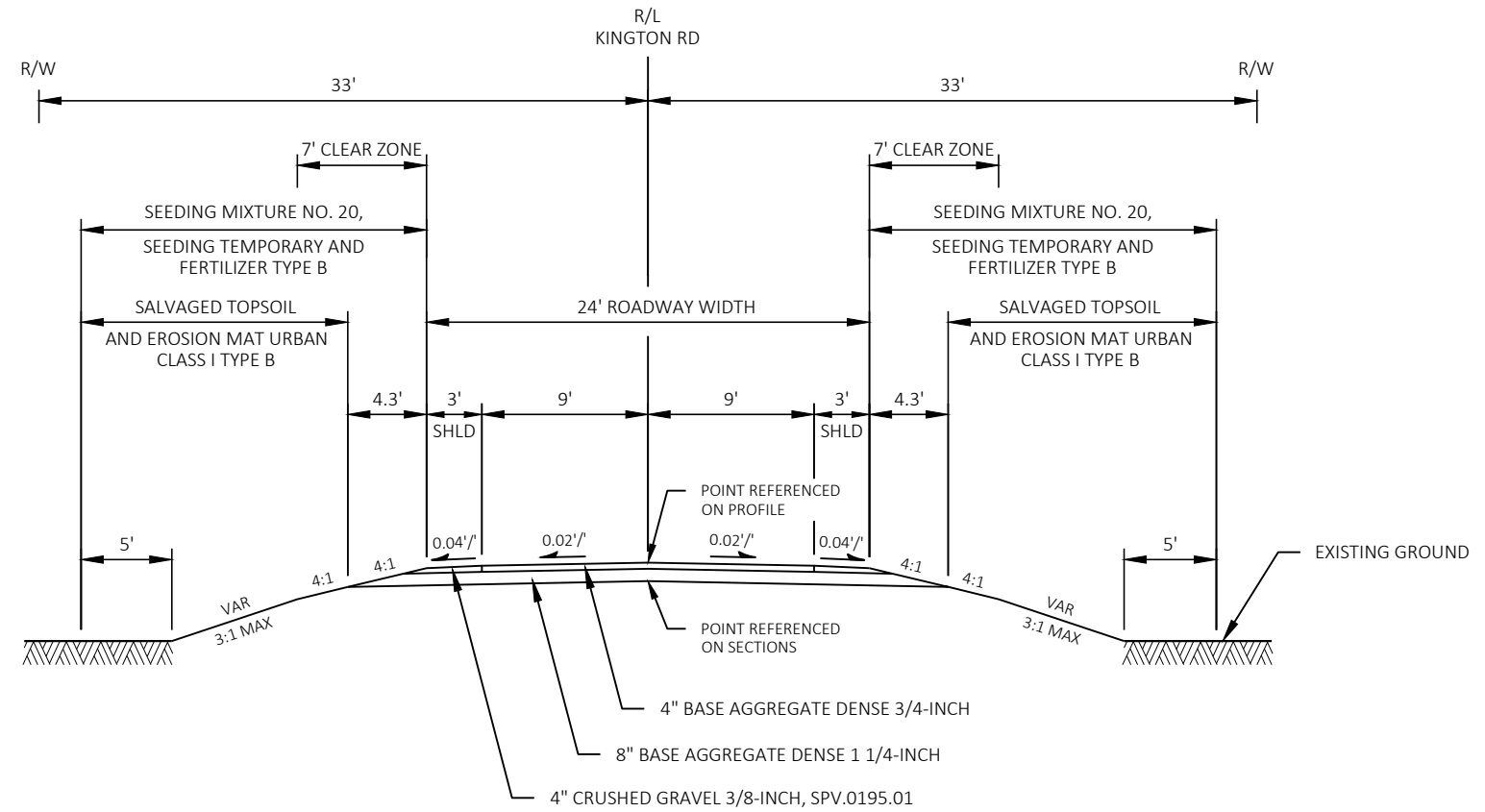
TOTAL PROJECT AREA = 0.23 ACRES
TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = 0.15 ACRES





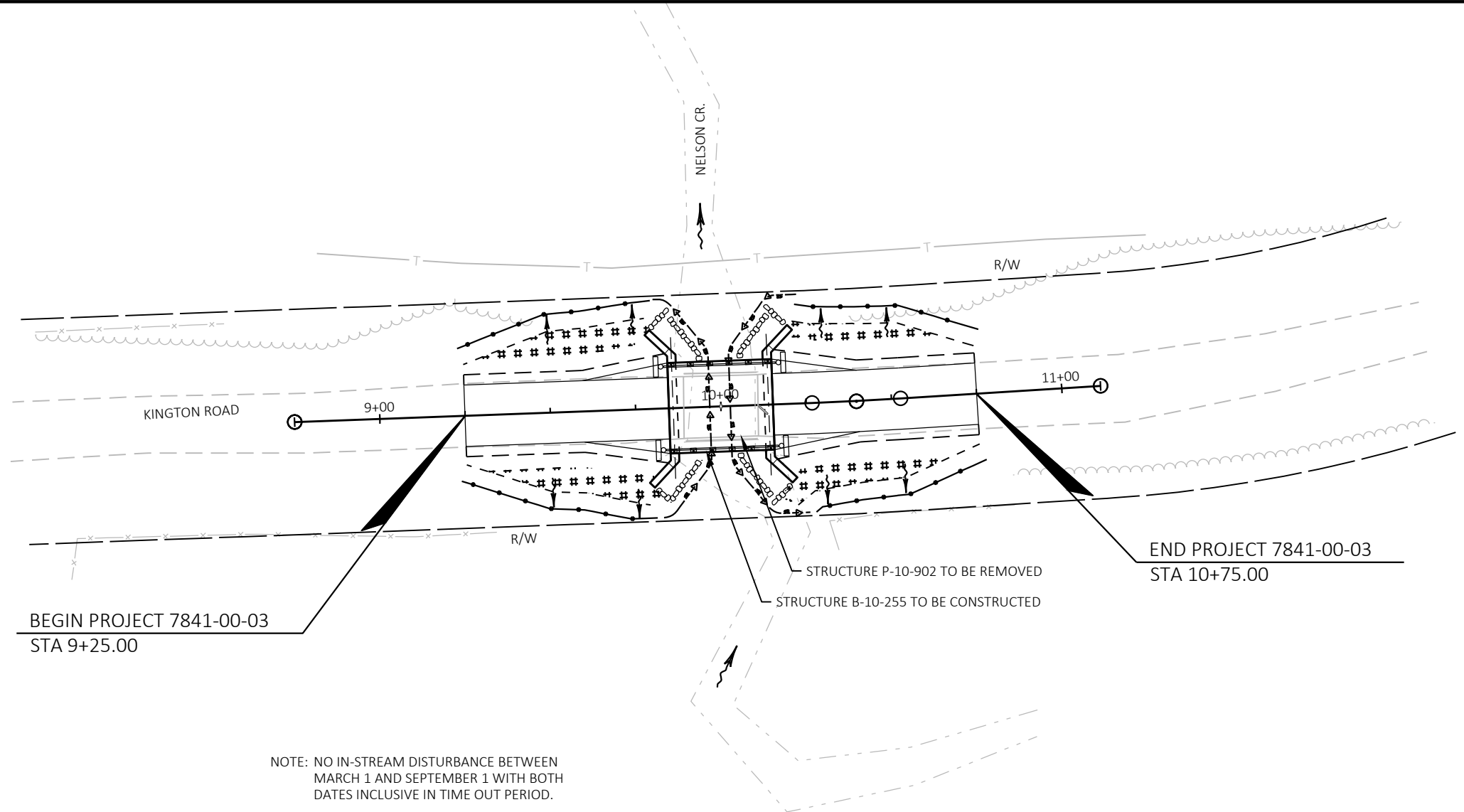
EXISTING TYPICAL SECTION

STA 9+25.0 TO STA 9+89.0
STA 10+10.0 TO STA 10+75.0



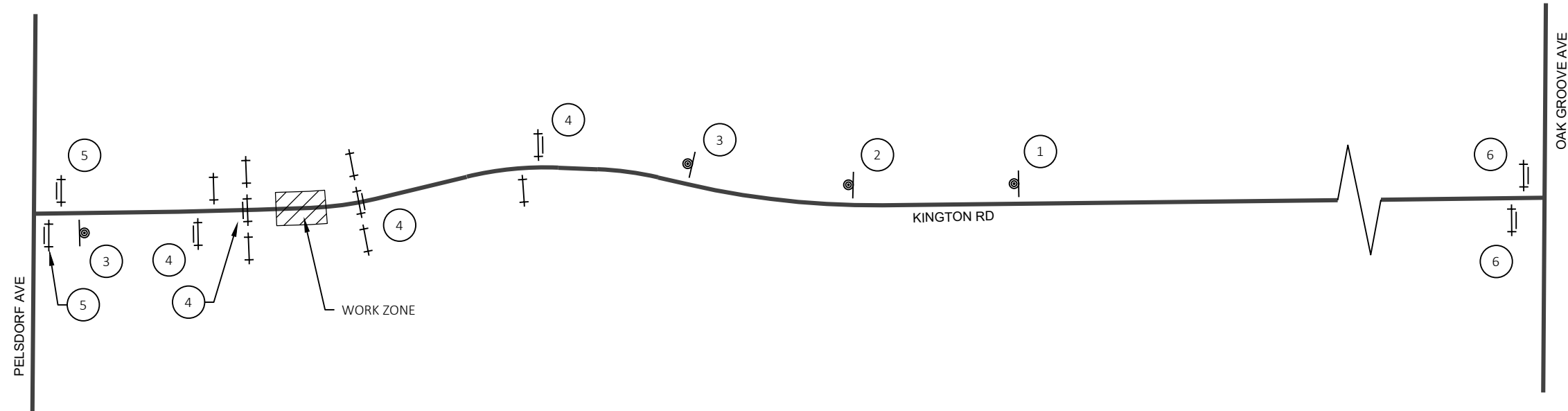
FINISHED TYPICAL SECTION

STA 9+25.00 TO STA 9+84.75
STA 10+15.25 TO STA 10+75.00







LEGEND

- ##### EROSION MAT URBAN CLASS I TYPE B
- SILT FENCE
- RIPRAP HEAVY
- - - SLOPE INTERCEPT
- ←←←←← TURBIDITY BARRIER
- ~> SURFACE WATER FLOW



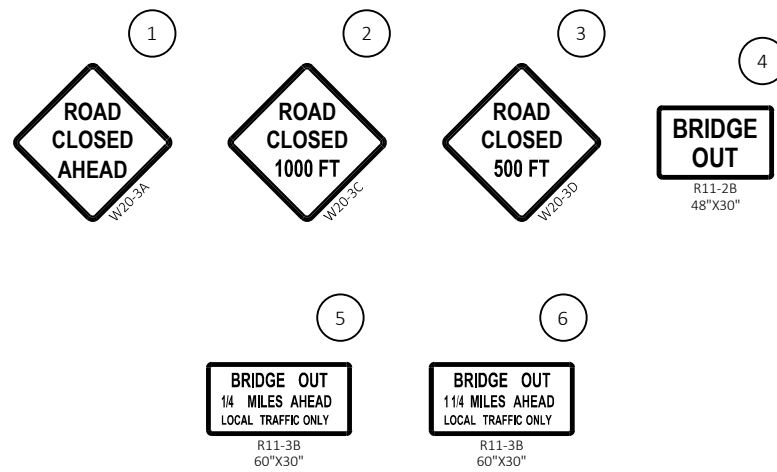
LEGEND

-  TYPE III BARRICADE
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  SIGN ON PERMANENT SUPPORT
-  WORK AREA

NOTES:

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ALL SIGNS ARE 48"X48" UNLESS NOTED.



Estimate Of Quantities

7841-00-03

Line	Item	Item Description	Unit	Total	Qty
0002	201.0205	Grubbing	STA	2.000	2.000
0004	203.0270	Removing Structure Over Waterway Debris Capture (structure) 01. P-10-902	EACH	1.000	1.000
0006	205.0100	Excavation Common	CY	97.000	97.000
0008	205.0505.S	Excavation, Hauling, and Disposal of Creosote Contaminated Soil and Management of Contaminated Groundwater	TON	85.000	85.000
0010	206.1001	Excavation for Structures Bridges (structure) 01. B-10-0255	EACH	1.000	1.000
0012	210.1500	Backfill Structure Type A	TON	400.000	400.000
0014	213.0100	Finishing Roadway (project) 01. 7841-00-03	EACH	1.000	1.000
0016	305.0110	Base Aggregate Dense 3/4-Inch	TON	80.000	80.000
0018	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	180.000	180.000
0020	502.0100	Concrete Masonry Bridges	CY	109.000	109.000
0022	502.3200	Protective Surface Treatment	SY	139.000	139.000
0024	505.0400	Bar Steel Reinforcement HS Structures	LB	4,220.000	4,220.000
0026	505.0600	Bar Steel Reinforcement HS Coated Structures	LB	12,660.000	12,660.000
0028	506.0105	Structural Steel Carbon	LB	540.000	540.000
0030	513.4061	Railing Tubular Type M	LF	66.000	66.000
0032	516.0500	Rubberized Membrane Waterproofing	SY	10.000	10.000
0034	550.0020	Pre-Boring Rock or Consolidated Materials	LF	160.000	160.000
0036	550.1100	Piling Steel HP 10-Inch X 42 Lb	LF	218.000	218.000
0038	606.0300	Riprap Heavy	CY	55.000	55.000
0040	612.0406	Pipe Underdrain Wrapped 6-Inch	LF	138.000	138.000
0042	618.0100	Maintenance and Repair of Haul Roads (project) 01. 7841-00-03	EACH	1.000	1.000
0044	619.1000	Mobilization	EACH	1.000	1.000
0046	624.0100	Water	MGAL	6.000	6.000
0048	625.0500	Salvaged Topsoil	SY	245.000	245.000
0050	628.1504	Silt Fence	LF	260.000	260.000
0052	628.1520	Silt Fence Maintenance	LF	520.000	520.000
0054	628.1905	Mobilizations Erosion Control	EACH	5.000	5.000
0056	628.1910	Mobilizations Emergency Erosion Control	EACH	2.000	2.000
0058	628.2008	Erosion Mat Urban Class I Type B	SY	245.000	245.000
0060	628.6005	Turbidity Barriers	SY	195.000	195.000
0062	629.0210	Fertilizer Type B	CWT	0.200	0.200
0064	630.0120	Seeding Mixture No. 20	LB	10.000	10.000
0066	630.0200	Seeding Temporary	LB	10.000	10.000
0068	630.0500	Seed Water	MGAL	8.000	8.000
0070	634.0614	Posts Wood 4x6-Inch X 14-FT	EACH	4.000	4.000
0072	637.2230	Signs Type II Reflective F	SF	12.000	12.000
0074	638.2602	Removing Signs Type II	EACH	4.000	4.000
0076	638.3000	Removing Small Sign Supports	EACH	4.000	4.000
0078	642.5001	Field Office Type B	EACH	1.000	1.000
0080	643.0420	Traffic Control Barricades Type III	DAY	1,260.000	1,260.000
0082	643.0705	Traffic Control Warning Lights Type A	DAY	2,160.000	2,160.000
0084	643.0900	Traffic Control Signs	DAY	1,080.000	1,080.000
0086	643.5000	Traffic Control	EACH	1.000	1.000
0088	645.0111	Geotextile Type DF Schedule A	SY	84.000	84.000
0090	645.0120	Geotextile Type HR	SY	83.000	83.000
0092	650.4500	Construction Staking Subgrade	LF	120.000	120.000
0094	650.5000	Construction Staking Base	LF	120.000	120.000
0096	650.6501	Construction Staking Structure Layout (structure) 01. B-10-0255	EACH	1.000	1.000
0098	650.9911	Construction Staking Supplemental Control (project) 01. 7841-00-03	EACH	1.000	1.000

Estimate Of Quantities

7841-00-03

Line	Item	Item Description	Unit	Total	Qty
0100	650.9920	Construction Staking Slope Stakes	LF	120.000	120.000
0102	715.0502	Incentive Strength Concrete Structures	DOL	2,010.000	2,010.000
0104	ASP.1T0A	On-the-Job Training Apprentice at \$5.00/HR	HRS	300.000	300.000
0106	ASP.1T0G	On-the-Job Training Graduate at \$5.00/HR	HRS	300.000	300.000
0108	SPV.0195	Special 01. Select Crushed Material for Travel Corridor	TON	31.000	31.000

GRUBBING

STATION	TO	STATION	LOCATION	201.0205 GRUBBING STA
9+25	-	10+75	KINGTON RD	2
TOTALS				2

BASE AGGREGATE DENSE

STATION	TO	STATION	LOCATION	305.0110 BASE AGGREGATE DENSE 3/4-INCH TON	305.0120 BASE AGGREGATE DENSE 1-1/4 INCH TON	624.0100 WATER MGAL
9+25	-	9+85	KINGTON RD, LT & RT	40	90	3
10+15	-	10+75	KINGTON RD, LT & RT	40	90	3
TOTALS				80	180	6

EARTHWORK SUMMARY

FROM/TO STATION	LOCATION	205.0100 EXCAVATION COMMON CUT (1)	SALVAGED/ UNUSABLE PAVEMENT MATERIAL	AVAILABLE MATERIAL (2)	UNEXPANDED FILL	EXPANDED FILL (FACTOR 1.25)	MASS ORDINATE +/- (3)	WASTE
9+25 - 10+75	KINGTON RD	91	0	91	48	60	31	31
TOTALS		91						

- (1) SALVAGED/UNUSABLE PAVEMENT MATERIAL IS INCLUDED
- (2) AVAILABLE MATERIAL = CUT - SALVAGED/UNUSABLE PAVEMENT MATERIAL
- (3) THE MASS ORDINATE + OR - QUANTITY CALCULATED. PLUS QUANTITY INDICATES AS EXCESS OF MATERIAL. MINUS INDICATES A SHORTAGE OF MATERIAL.

TOTAL WASTE = 38

LANDSCAPING ITEMS

LOCATION	619.1000 MOBILIZATION EACH	STATION	TO	STATION	LOCATION	625.0500 SALVAGED TOPSOIL SY	628.2008 EROSION MAT URBAN CLASS I TYPE B SY	629.0210 FERTILIZER TYPE B CWT	630.0120 SEEDING MIXTURE NO. 20 LB	630.0200 SEEDING TEMPORARY LB	630.0500 SEED WATER MGAL	REMARKS
KINGTON RD	1	9+25	-	9+85	KINGTON RD, LT & RT	125	125	0.1	5	5	4	
		10+15	-	10+75	KINGTON RD, LT & RT	120	120	0.1	5	5	4	
TOTALS						245	245	0.2	10	10	8	

EROSION CONTROL SUMMARY

STATION	TO	STATION	LOCATION	628.1905 MOBILIZATIONS EROSION CONTROL EACH	628.1910 MOBILIZATIONS EMERGENCY EROSION CONTROL EACH
UNDISTRIBUTED		VARIOUS		5	2
TOTAL				5	2

SILT FENCE

STATION	TO	STATION	LOCATION	628.1504 SILT FENCE LF	628.1520 SILT FENCE MAINTENANCE LF
9+25	-	9+85	KINGTON RD, LT & RT	115	230
10+15	-	10+75	KINGTON RD, LT & RT	110	220
UNDISTRIBUTED				35	70
TOTAL				260	520

TURBIDITY BARRIERS

STATION	TO	STATION	LOCATION	628.6005 TURBIDITY BARRIERS SY
9+97			KINGTON RD	90
10+03			KINGTON RD	105
TOTALS				195

PROJECT NO: 7841-00-03

HWY: LOCAL STREET

COUNTY: CLARK

MISCELLANEOUS QUANTITIES

SHEET

E

SIGNING

STATION	LOCATION	634.0614 POSTS WOOD 4x6-INCH x 14-FT EACH	637.2230 SIGNS TYPE II REFLECTIVE F SF	638.2602 REMOVING SIGNS TYPE II EACH	638.3000 REMOVING SMALL SIGN SUPPORTS EACH	COMMENTS
9+83	KINGTON RD, LT & RT	2	6	-	-	W5-52L & W5-52R
9+85	KINGTON RD, LT & RT	-	-	2	2	W5-52L & W5-52R
10+15	KINGTON RD, LT & RT	-	-	2	2	W5-52L & W5-52R
10+17	KINGTON RD, LT & RT	2	6	-	-	W5-52L & W5-52R
TOTALS		4	12	4	4	

TRAFFIC CONTROL ITEMS

PROJECT	TRAFFIC CONTROL BARRICADES TYPE III EACH	643.0420 TRAFFIC CONTROL BARRICADES TYPE III DAY	TRAFFIC CONTROL WARNING LIGHTS TYPE A EACH	643.0705 TRAFFIC CONTROL WARNING LIGHTS TYPE A DAY	TRAFFIC CONTROL SIGNS EACH	643.0900 TRAFFIC CONTROL SIGNS DAY	643.5000 TRAFFIC CONTROL EACH	REMARKS
7841-00-03	14	1,260	24	2,160	12	1,080	1.0	90 DAYS
TOTALS		1,260		2,160		1,080	1.0	

CONSTRUCTION STAKING

CATEGORY	STATION	TO	STATION	LOCATION	650.4500 CONSTRUCTION STAKING SUBGRADE LF	650.5000 CONSTRUCTION STAKING BASE LF	650.6501 CONSTRUCTION STAKING STRUCTURE LAYOUT (B-10-255) EACH	650.9911 CONSTRUCTION STAKING SUPPLEMENTAL CONTROL (PROJECT) EACH	650.9920 CONSTRUCTION STAKING SLOPE STAKES LF
0010	9+25	-	9+85	KINGTON RD	60	60	-	-	60
0010	10+15	-	10+75	KINGTON RD	60	60	-	-	60
0020		10+00.00		KINGTON RD	-	-	1	-	-
0010		PROJECT		KINGTON RD	-	-	-	1	-
TOTALS					120	120	1	1	120

CREOSOTE CONTAMINATED SOIL

205.0505.S
EXCAVATION, HAULING,
AND DISPOSAL OF
CREOSOTE
CONTAMINATED SOIL AND
MANAGEMENT OF
CONTAMINATED
GROUNDWATER

NOTE: ALL ITEMS AND QUANTITIES ON THIS SHEET ARE CATEGORY 0010 UNLESS OTHERWISE NOTED.

CATEGORY	LOCATION	TON
0020	KINGTON RD	85
TOTALS		85

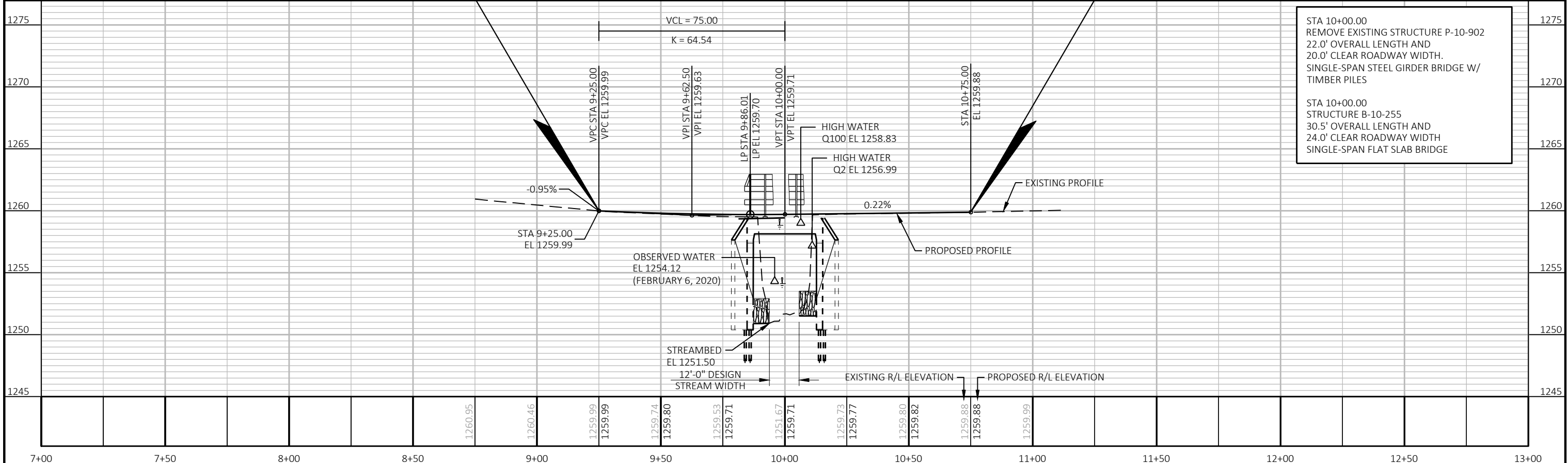
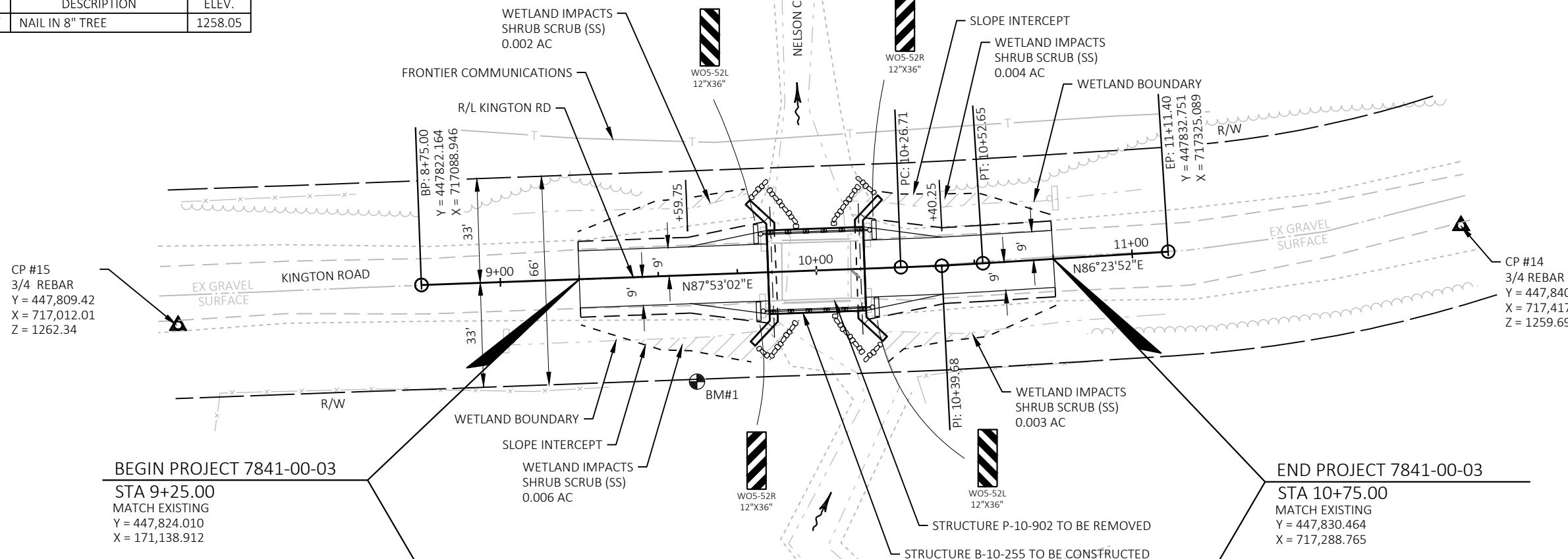
BENCH MARKS			
NO.	STATION	DESCRIPTION	ELEV.
1	9+60.9, RT 33.6'	NAIL IN 8" TREE	1258.05

CP #15
3/4 REBAR
Y = 447,809.42
X = 717,012.01
Z = 1262.34

PI STA = 10+39.68
Y = 447828.245
X = 717253.518
DELTA = 1°29'10" LT
D = 5°43'46"
T = 12.97'
L = 25.94'
R = 1000.00'
PC STA = 10+26.71
Y = 447827.766
X = 717240.557
PT STA = 10+52.65
Y = 447829.060
X = 717266.463

BEGIN PROJECT 7841-00-03
STA 9+25.00
MATCH EXISTING
Y = 447,824.010
X = 171,138.912

END PROJECT 7841-00-03
STA 10+75.00
MATCH EXISTING
Y = 447,830.464
X = 717,288.765



STA 10+00.00
REMOVE EXISTING STRUCTURE P-10-902
22.0' OVERALL LENGTH AND
20.0' CLEAR ROADWAY WIDTH.
SINGLE-SPAN STEEL GIRDER BRIDGE W/
TIMBER PILES

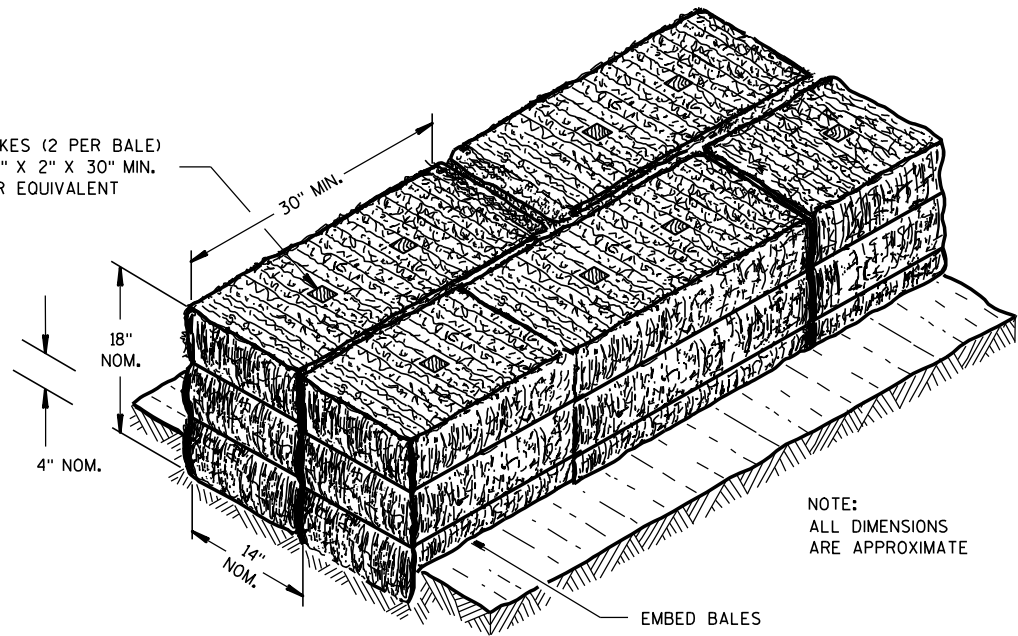
STA 10+00.00
STRUCTURE B-10-255
30.5' OVERALL LENGTH AND
24.0' CLEAR ROADWAY WIDTH
SINGLE-SPAN FLAT SLAB BRIDGE

PROJECT NO: 7841-00-03	HWY: LOCAL STREET	COUNTY: CLARK	PLAN AND PROFILE: KINGTON RD	SHEET	E
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Standard Detail Drawing List

08E08-03	TYPICAL INSTALLATIONS OF EROSION BALES / TEMPORARY DITCH CHECKS
08E09-06	SILT FENCE
08E11-02	TURBIDITY BARRIER
12A03-10	NAME PLATE (STRUCTURES)
15C02-09A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C05-05	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M. P. H. OR LESS

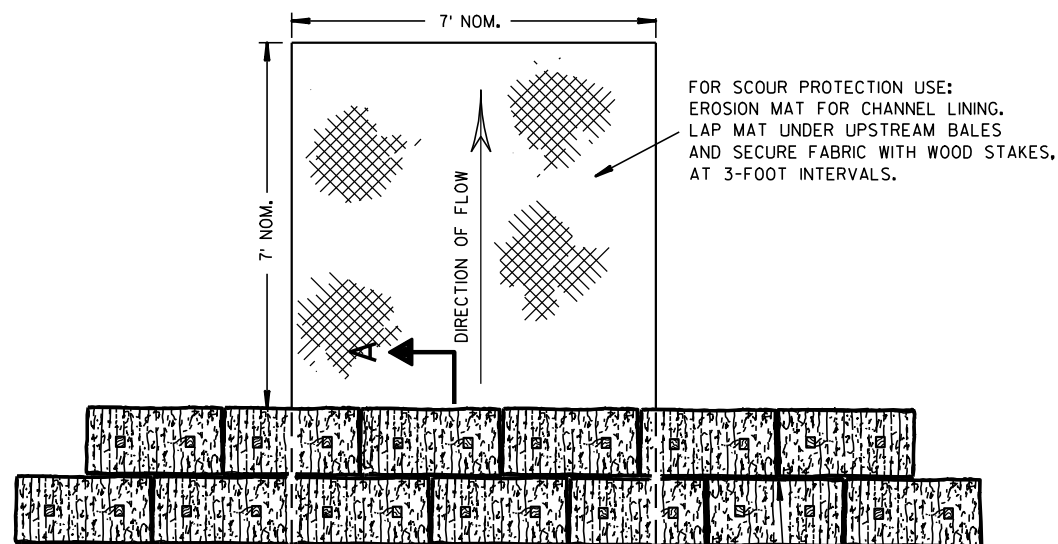
WOOD STAKES (2 PER BALE)
NOMINAL 2" X 2" X 30" MIN.
LENGTH OR EQUIVALENT



NOTE:
ALL DIMENSIONS
ARE APPROXIMATE

EMBED BALES

SECTION A-A

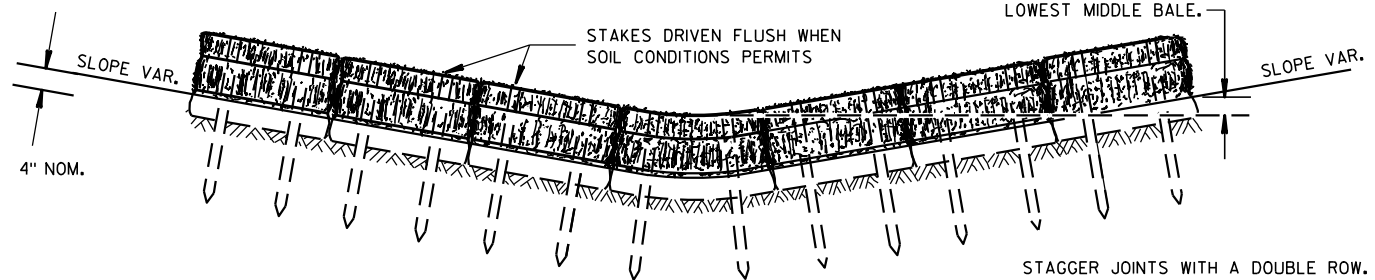


FOR SCOUR PROTECTION USE:
EROSION MAT FOR CHANNEL LINING.
LAP MAT UNDER UPSTREAM BALES
AND SECURE FABRIC WITH WOOD STAKES,
AT 3-FOOT INTERVALS.

STAGGER JOINTS BETWEEN ADJACENT
ROWS OF BALES.

PLAN VIEW

BOTTOM ELEVATION OF END BALE SHALL
BE EQUAL TO OR GREATER THAN TOP OF
LOWEST MIDDLE BALE.



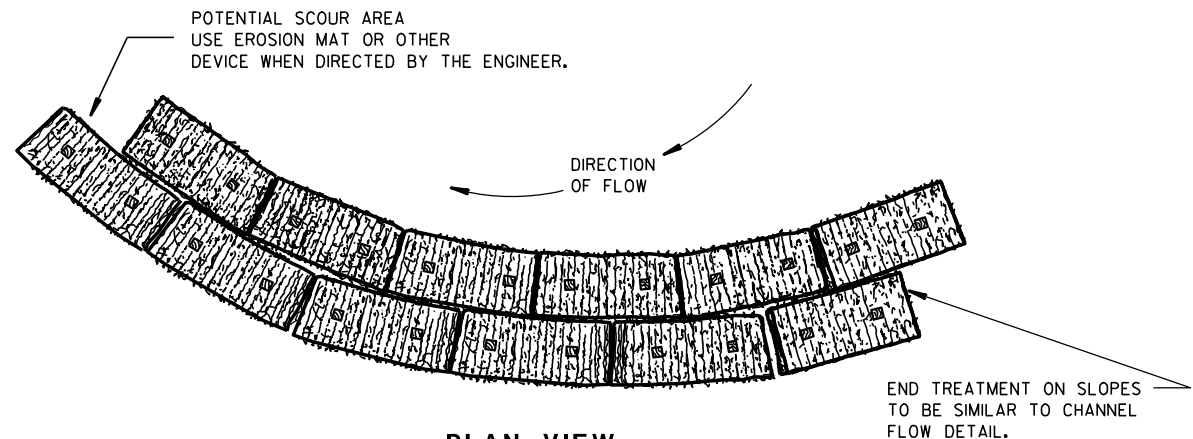
FRONT ELEVATION

TEMPORARY DITCH CHECK USING EROSION BALES ①

GENERAL NOTES

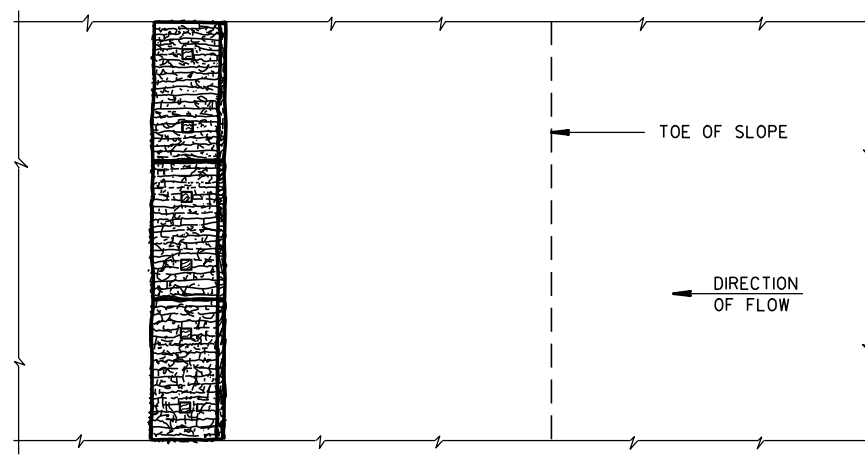
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

- ① TEMPORARY DITCH CHECKS EITHER EROSION BALES OR MANUFACTURED SHALL BE PAID FOR UNDER THE BID ITEM OF TEMPORARY DITCH CHECK. THE DEPARTMENT WILL NOT PAY FOR TEMPORARY DITCH CHECKS CONSTRUCTED OF A SINGLE ROW OF EROSION BALES.

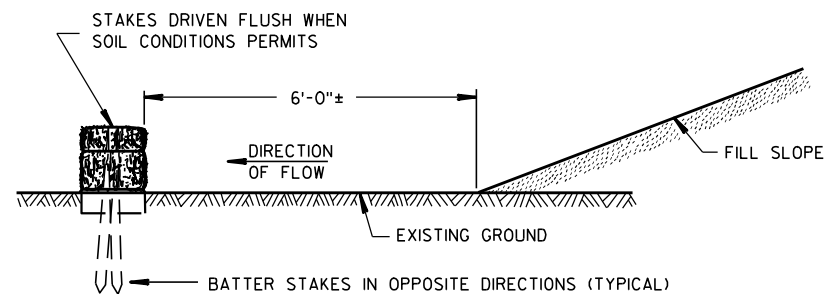


PLAN VIEW

WHEN ALTERING THE DIRECTION OF FLOW



PLAN VIEW



FRONT ELEVATION

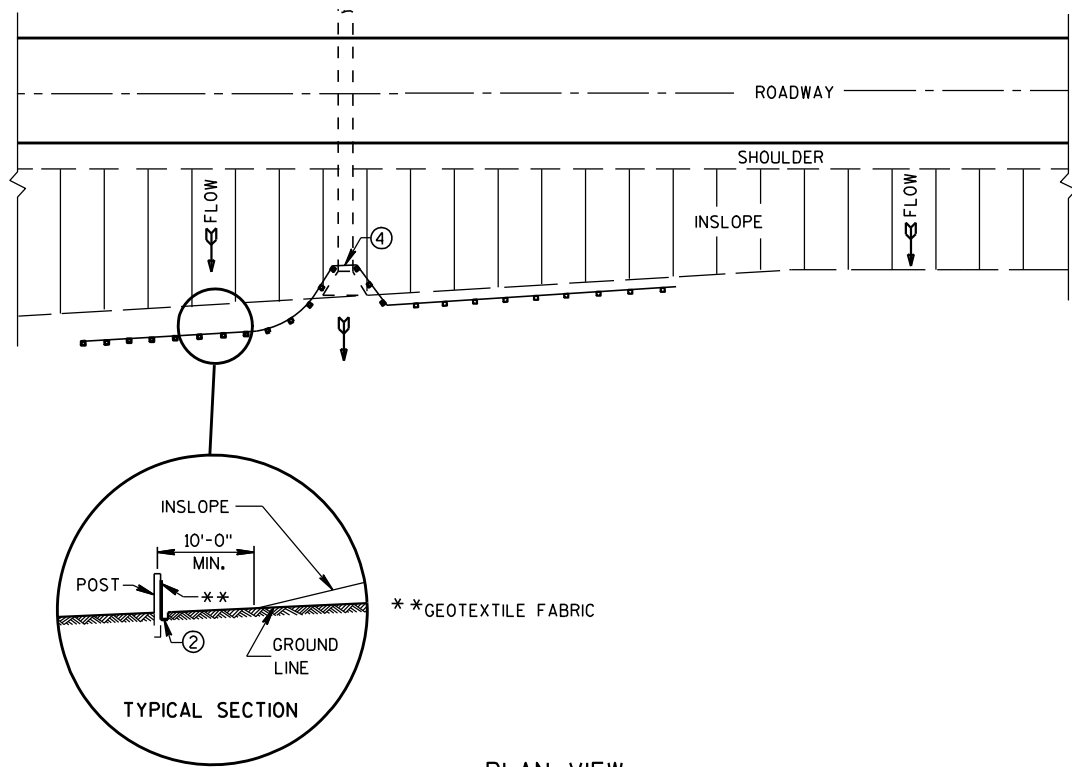
WHEN EXISTING GROUND SLOPES AWAY FROM FILL SLOPE

EROSION BALES FOR SHEET FLOW

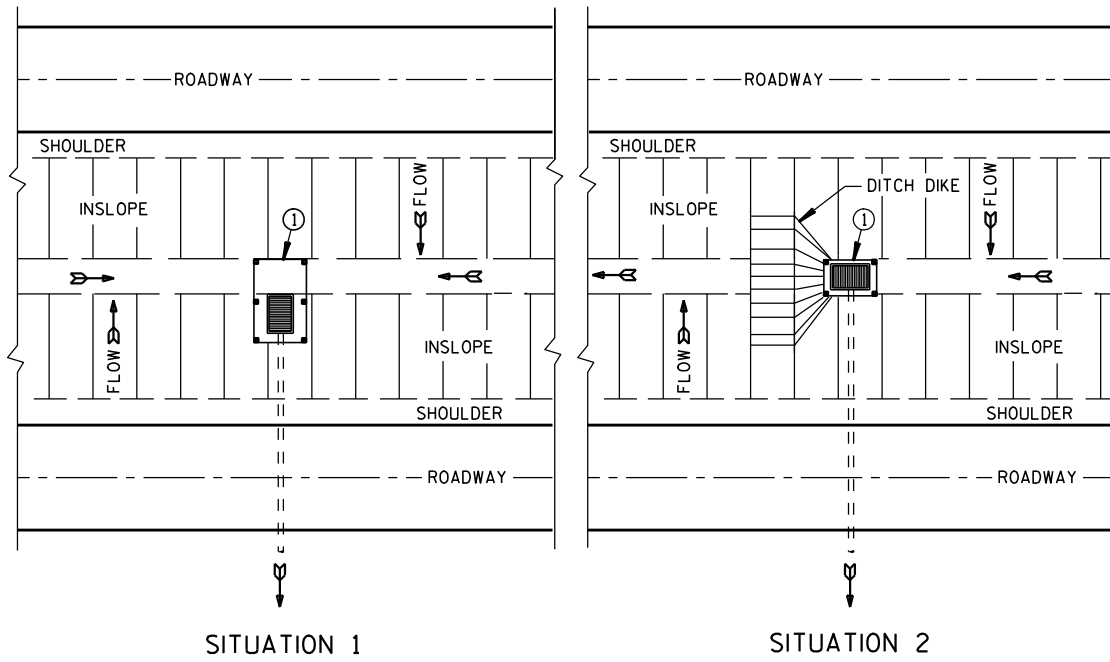
TYPICAL INSTALLATIONS OF
EROSION BALES / TEMPORARY
DITCH CHECKS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
 6/04/02 /S/ Beth Canestra
 DATE CHIEF ROADWAY DEVELOPMENT ENGINEER
 FHWA



PLAN VIEW
TYPICAL APPLICATION OF SILT FENCE

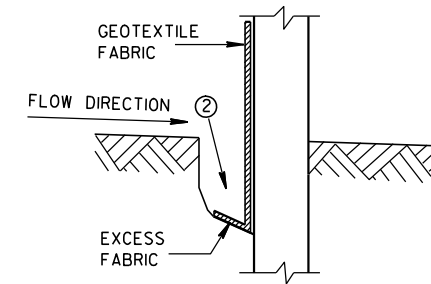


SITUATION 1 SITUATION 2
PLAN VIEW
SILT FENCE AT MEDIAN SURFACE DRAINS

GENERAL NOTES

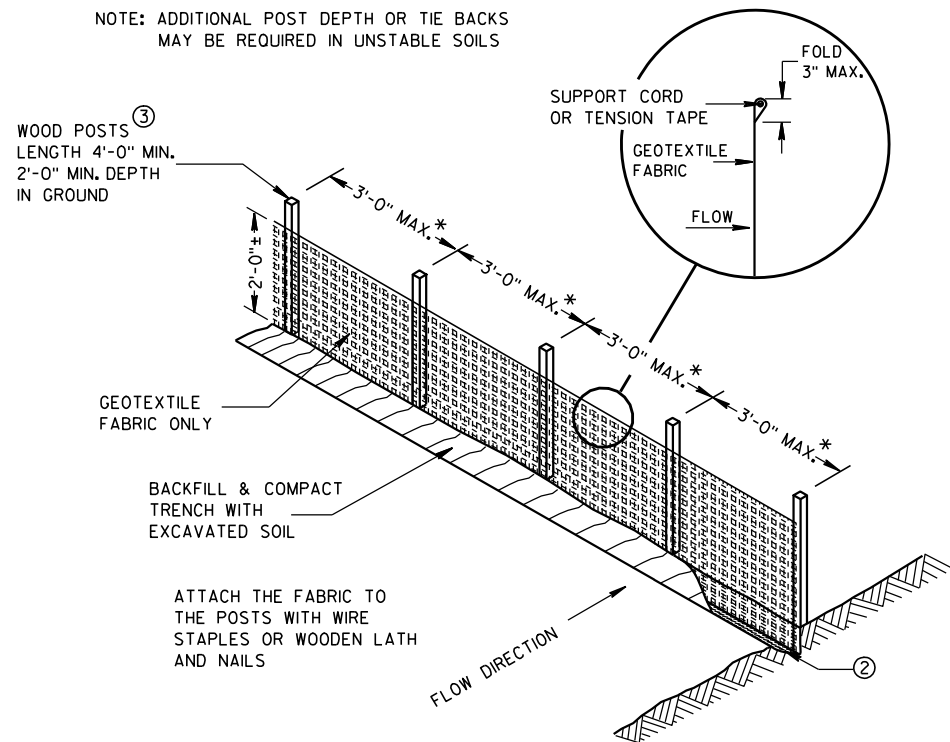
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

- ① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- ③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1 1/8" X 1 1/8" OF OAK OR HICKORY.
- ④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- ⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



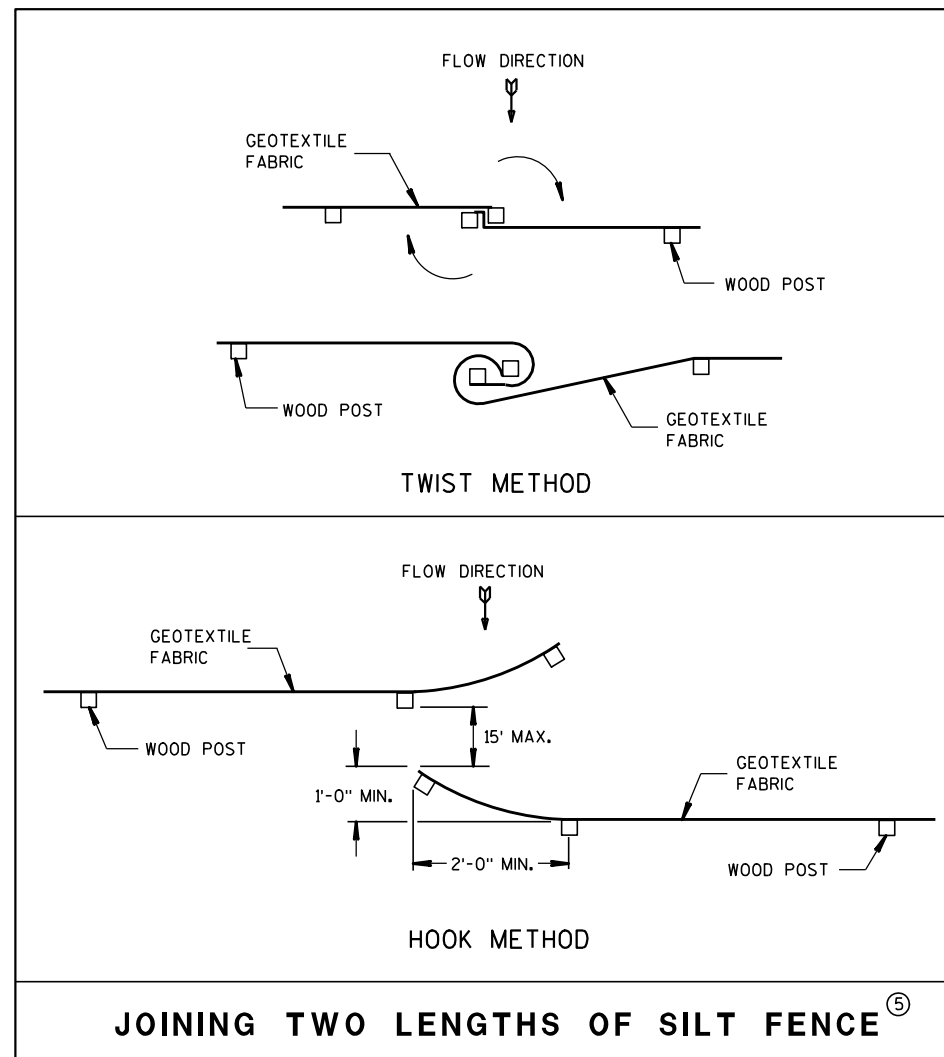
TRENCH DETAIL

NOTE: ADDITIONAL POST DEPTH OR TIE BACKS MAY BE REQUIRED IN UNSTABLE SOILS

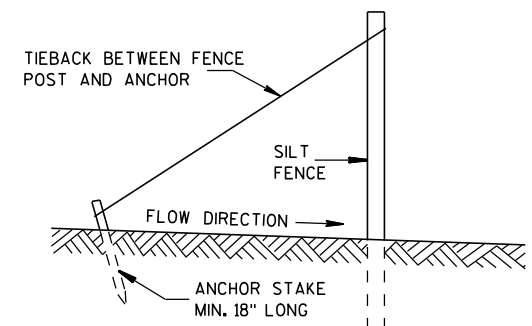


SILT FENCE

* NOTE: 8'-0" POST SPACING ALLOWED IF A WOVEN GEOTEXTILE FABRIC IS USED.



JOINING TWO LENGTHS OF SILT FENCE

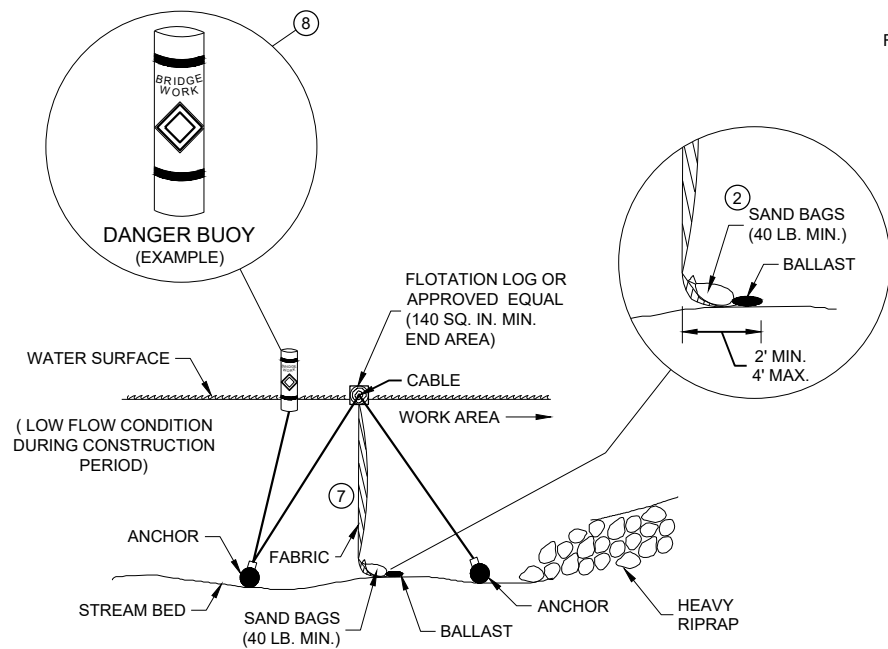


SILT FENCE TIE BACK
(WHEN REQUIRED BY THE ENGINEER)

SILT FENCE

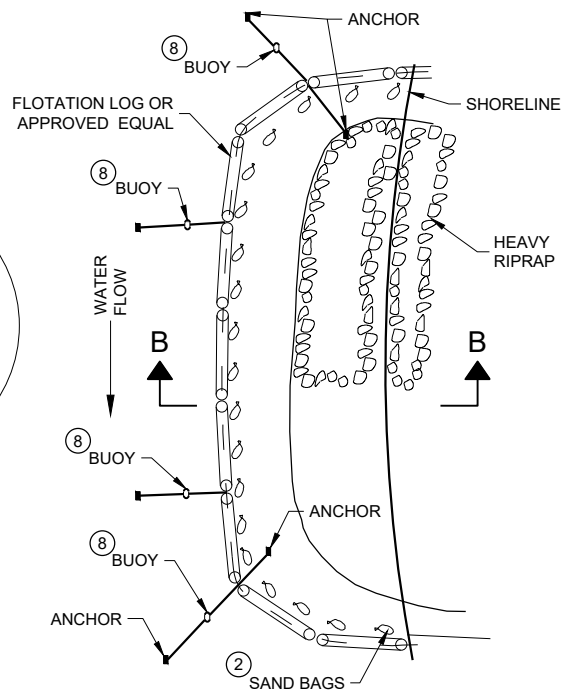
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
4-29-05 /S/ Beth Canestra
DATE CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA

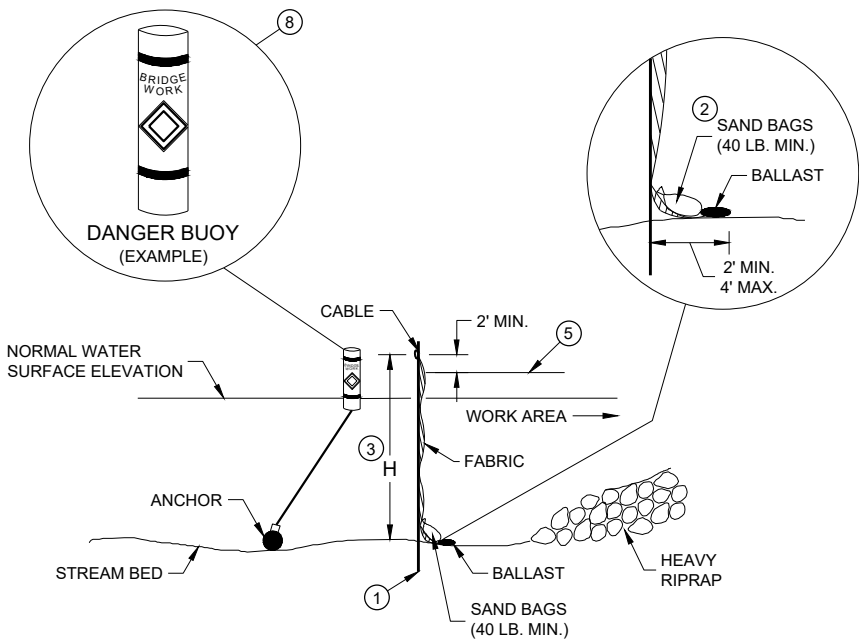


SECTION B - B

**TURBIDITY BARRIER - FLOAT ALTERNATIVE
CAUTION - SEE NOTE 6**

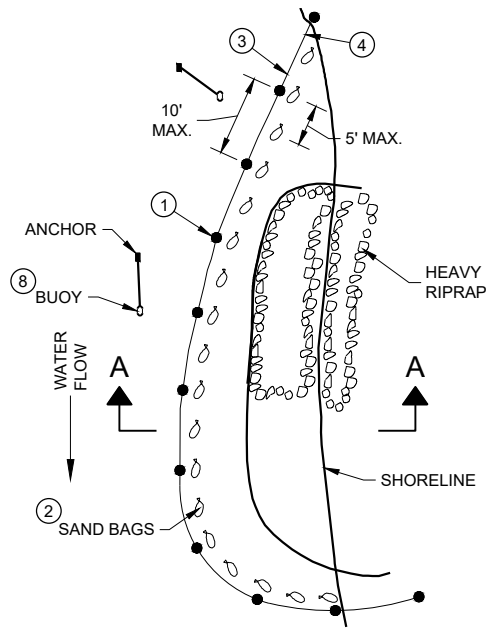


PLAN VIEW



SECTION A - A

TURBIDITY BARRIER - STANDARD POST INSTALLATION



PLAN VIEW

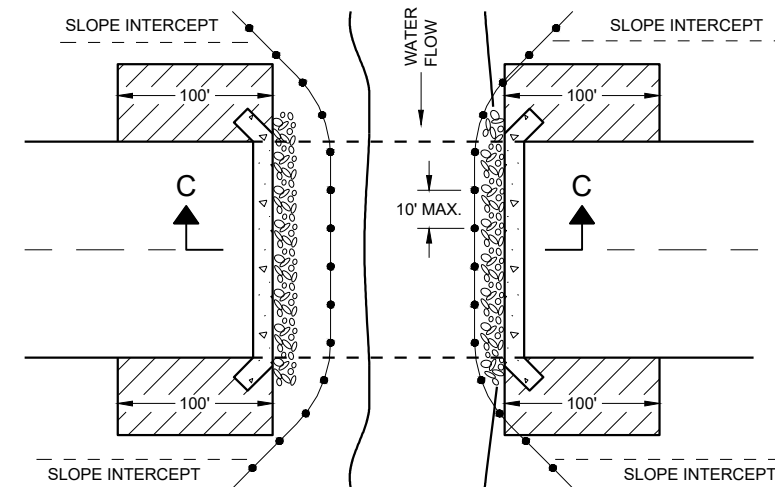
TURBIDITY BARRIER PLACEMENT DETAILS

GENERAL NOTES

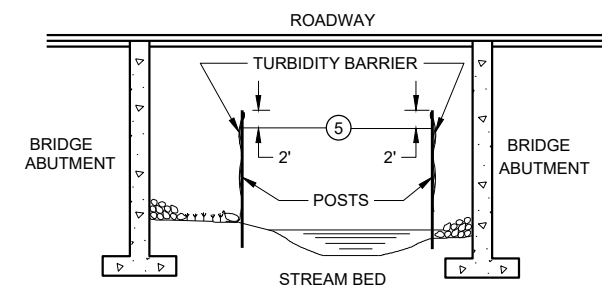
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

TURBIDITY BARRIER MAY BE REMOVED AT THE ENGINEERS DISCRETION, WHEN PERMANENT EROSION CONTROL MEASURES HAVE BEEN ESTABLISHED.

- ① DRIVEN STEEL POSTS, PIPES, OR CHANNELS. LENGTH SHALL BE SUFFICIENT TO SECURELY SUPPORT BARRIER AT HIGH WATER ELEVATIONS.
- ② SAND BAGS TO BE USED AS ADDITIONAL BALLAST WHEN ORDERED BY THE ENGINEER TO MEET ADVERSE FIELD CONDITIONS. SPACE AS APPROPRIATE FOR SITE CONDITIONS.
- ③ WHEN BARRIER HEIGHT "H" EXCEEDS 8 FEET, POST SPACING MAY NEED TO BE DECREASED.
- ④ IN WATERWAYS SUBJECT TO FLUCTUATING WATER ELEVATIONS, PROVISIONS SHOULD BE MADE TO ALLOW THE WATER TO EQUALIZE ON EACH SIDE OF THE BARRIER. THIS MAY BE ACCOMPLISHED BY LEAVING A PORTION OF THE BARRIER OPEN ON THE UPSTREAM END.
- ⑤ ESTIMATED HIGH WATER ELEVATION DURING CONSTRUCTION PERIOD. MINIMUM BARRIER HEIGHT SHALL BE 2' GREATER THAN EITHER THE Q2 ELEVATION OR THE ESTIMATED HIGH WATER ELEVATION DURING CONSTRUCTION, WHICHEVER IS GREATER.
- ⑥ FLOAT ALTERNATIVE WILL ONLY BE ALLOWED WITH WRITTEN APPROVAL OF THE ENGINEER, AND IS MEANT FOR LOCATIONS WHERE BEDROCK PREVENTS THE INSTALLATION OF POSTS.
- ⑦ ALLOW SUFFICIENT SLACK VERTICALLY AND HORIZONTALLY SO THAT SEDIMENT BUILD UP WILL NOT SEPARATE OR LOWER THE TURBIDITY BARRIER.
- ⑧ USE AS DIRECTED BY COAST GUARD OR DNR PERMIT WHEN WORKING IN NAVIGABLE WATERWAYS.



PLAN VIEW



SECTION C - C

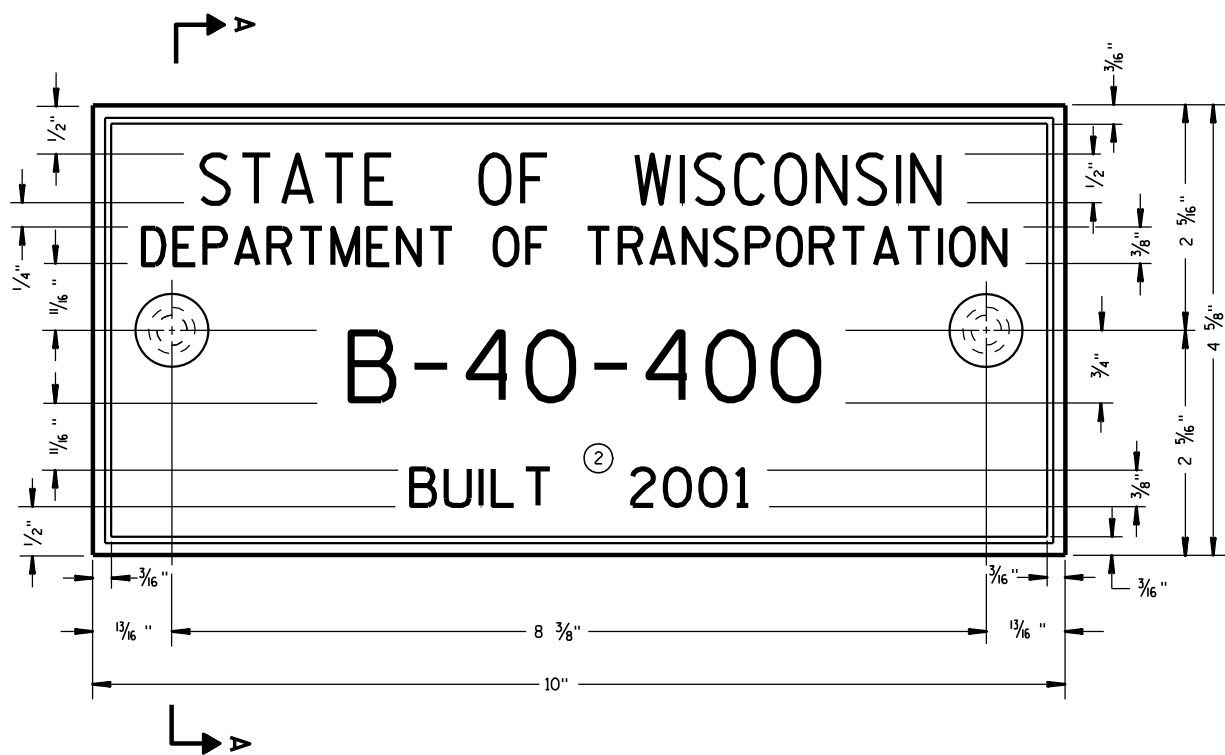
**TURBIDITY BARRIER DETAIL SHOWING
TYPICAL PLACEMENT AT STRUCTURES**

TURBIDITY BARRIER

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
6/4/02 DATE /S/ Beth Cannestra
DATE CHIEF ROADWAY DEVELOPMENT
ENGINEER

FHWA



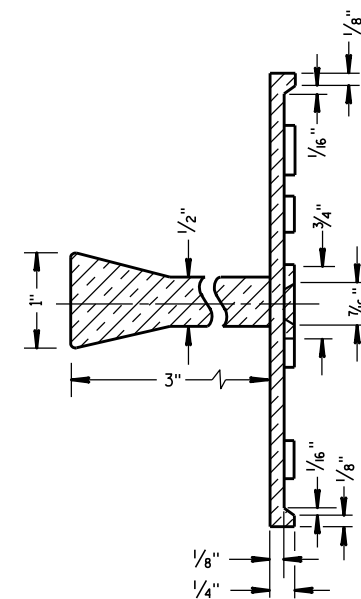
TYPICAL NAME PLATE
(BRIDGES, CULVERTS, AND RETAINING WALLS)

GENERAL NOTES

NAME PLATES TO BE INSTALLED ON BRIDGES, CULVERTS, AND RETAINING WALLS SHALL CONFORM TO THE REQUIREMENTS OF SECTION 502.3.11 OF THE STANDARD SPECIFICATIONS.

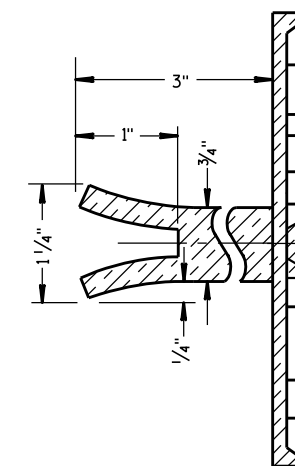
THE BRIDGE NUMBER AND YEAR BUILT SHOWN ON THIS DRAWING ARE EXAMPLES ONLY. SEE CONSTRUCTION PLANS FOR INDIVIDUAL NUMBERING AND YEAR BUILT.

- ① EPOXY RESIN SHALL BE FROM AN APPROVED MANUFACTURER AND USED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS.
- ② REHABILITATION OF AN EXISTING STRUCTURE SHOULD USE THE DATE OF ORIGINAL STRUCTURE CONSTRUCTION.

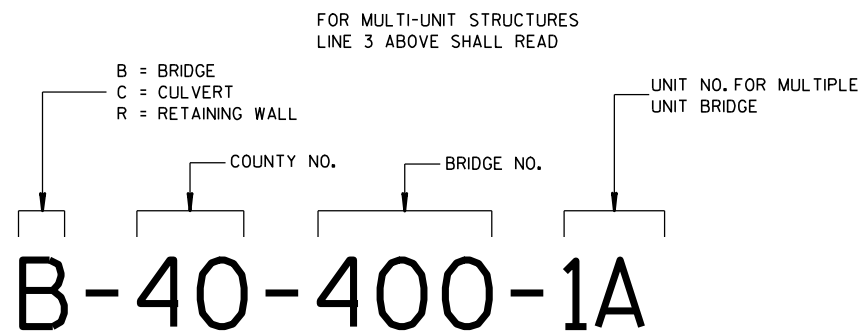


SECTION A-A

SPREAD OPEN SO THE TOP OF LUG IS 1 1/4" WIDE

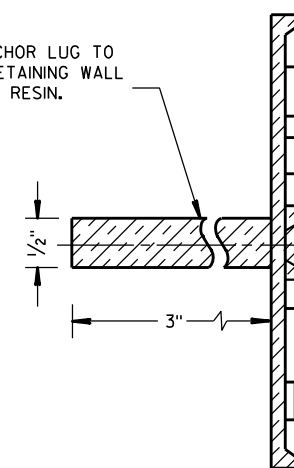


ALTERNATE LUG



**NUMBERING DESIGNATION
MULTI-UNIT STRUCTURES**

- ① ADHERE ANCHOR LUG TO PRECAST RETAINING WALL WITH EPOXY RESIN.

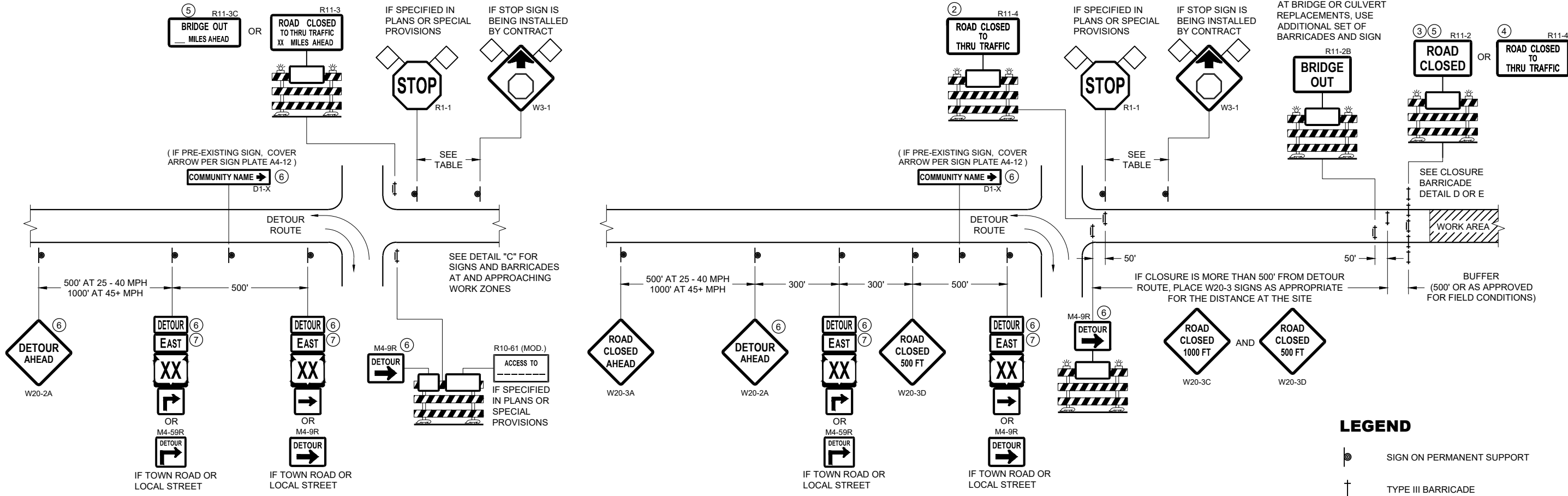


ALTERNATE LUG
(FOR ATTACHMENT TO PRECAST STRUCTURES)

**NAME PLATE
(STRUCTURES)**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE 3/26/10 /S/ Scot Becker
CHIEF STRUCTURAL DEVELOPMENT ENGINEER
FHWA



**DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR**

WORK ZONE GREATER THAN OR EQUAL TO 1/2 MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)

**DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR**

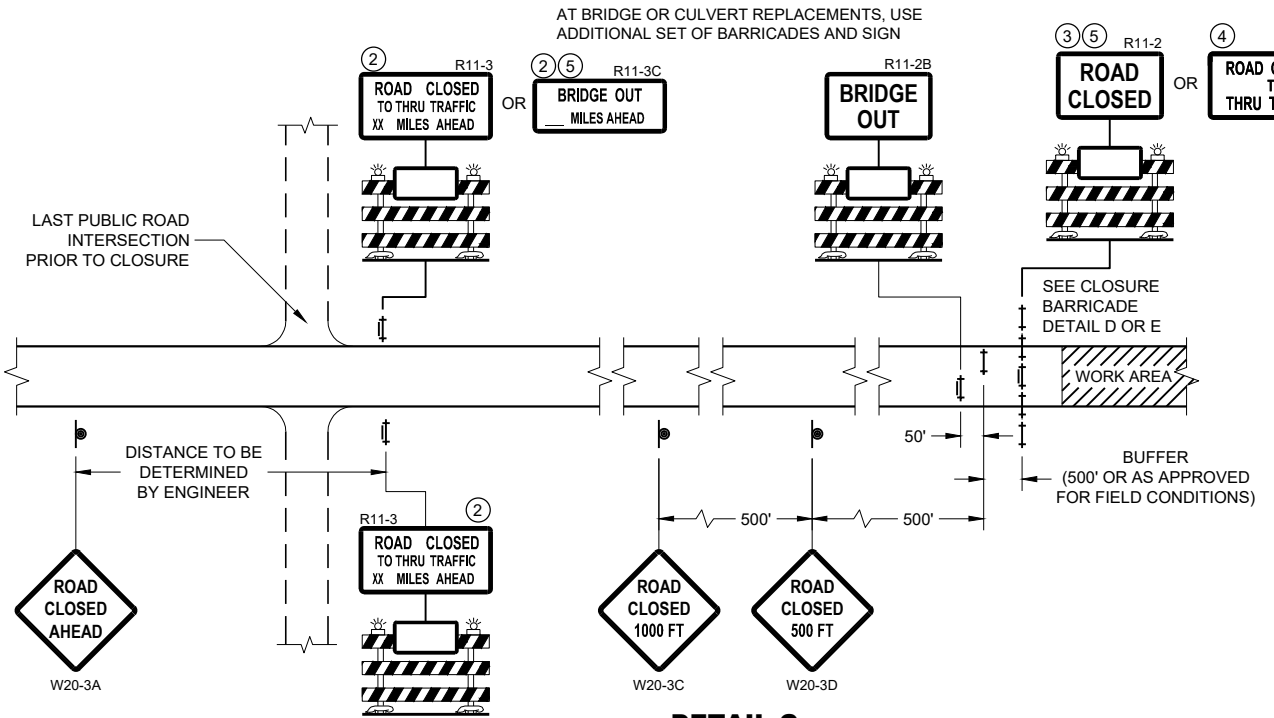
WORK ZONE LESS THAN 1/2 MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
- FLAGS, 16" X 16" MIN. (ORANGE)

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

- M4 - 8
- M3 - X
- M1 - 4
- M1 - 6
- M1 - 5A
- M05 - 1
- M06 - 1



**DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR**

SEE SDD 15C2-SHEET "b"
FOR GENERAL NOTES
AND FOOTNOTES ① THROUGH ⑦

**BARRICADES AND SIGNS
FOR MAINLINE CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

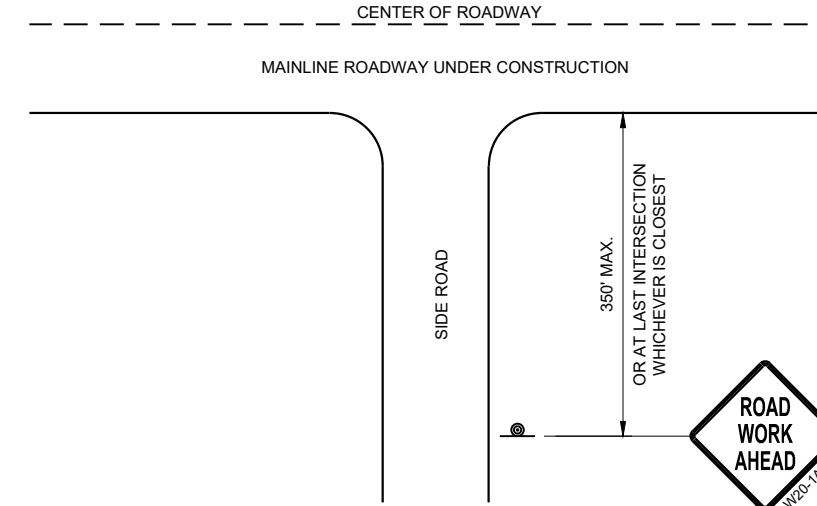
THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS, 36"X36" SIGNS MAY BE USED INSTEAD OF 48" X 48" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

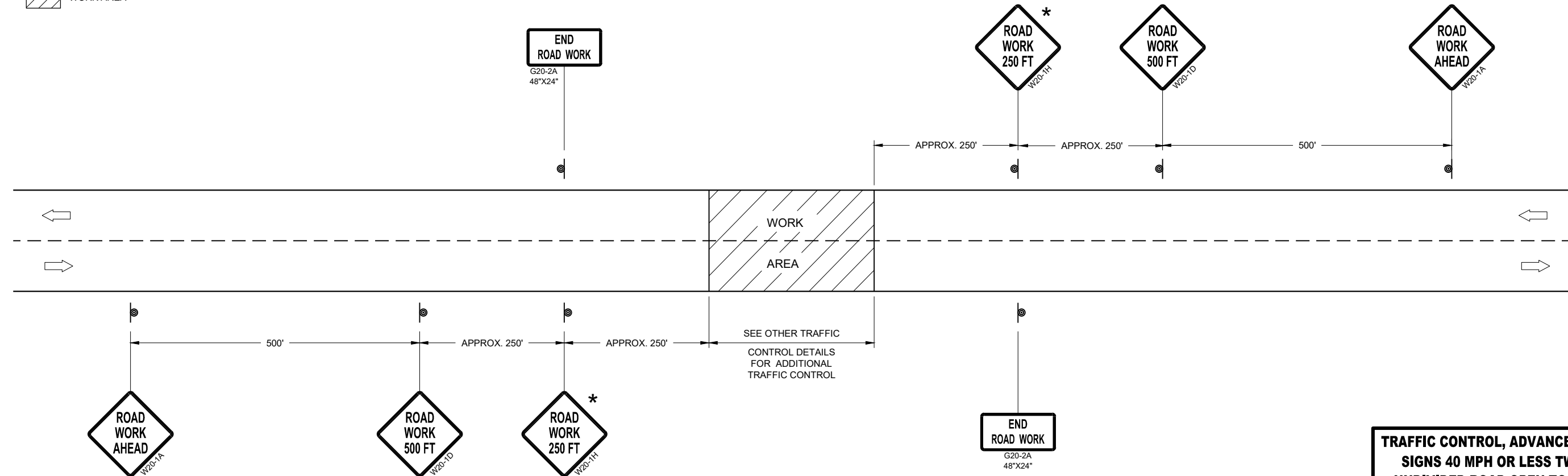
* THE THIRD W20-1 SIGN IS REQUIRED ONLY IF THERE IS AN INTERSECTION BETWEEN THE "ROAD WORK 500 FEET" SIGN AND THE WORK ZONE. ADJUST THE PLACEMENT OF THIS SIGN BASED ON INTERSECTION LOCATION AND OTHER FIELD CONDITIONS.



**TYPICAL SIDE ROAD APPROACH
WARNING SIGN DETAIL**

LEGEND

- SIGN ON PERMANENT SUPPORT
- DIRECTION OF TRAFFIC
- WORK AREA



TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40MPH OR LESS

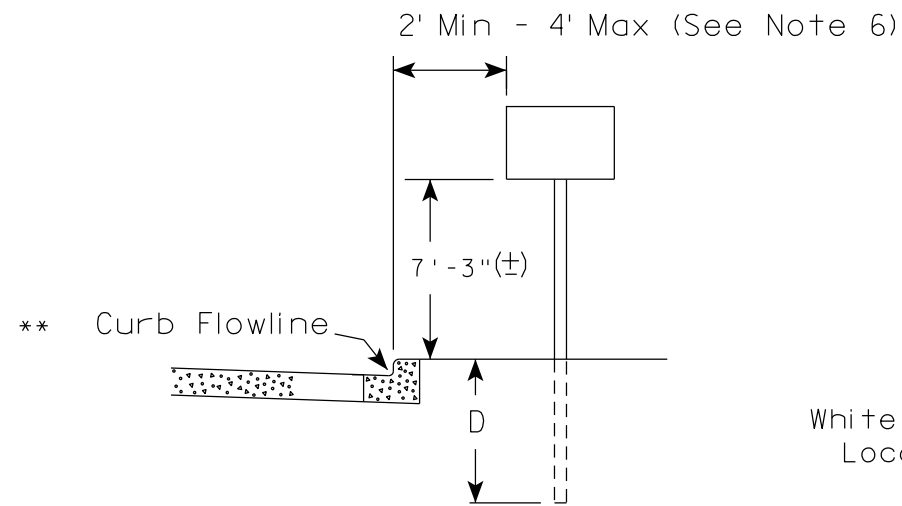
**TRAFFIC CONTROL, ADVANCE WARNING
SIGNS 40 MPH OR LESS TWO-WAY
UNDIVIDED ROAD OPEN TO TRAFFIC**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

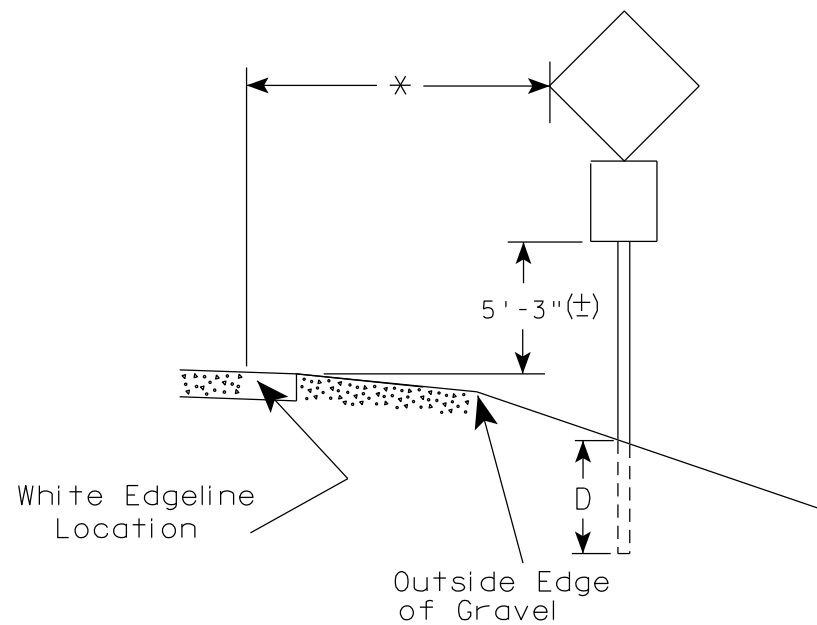
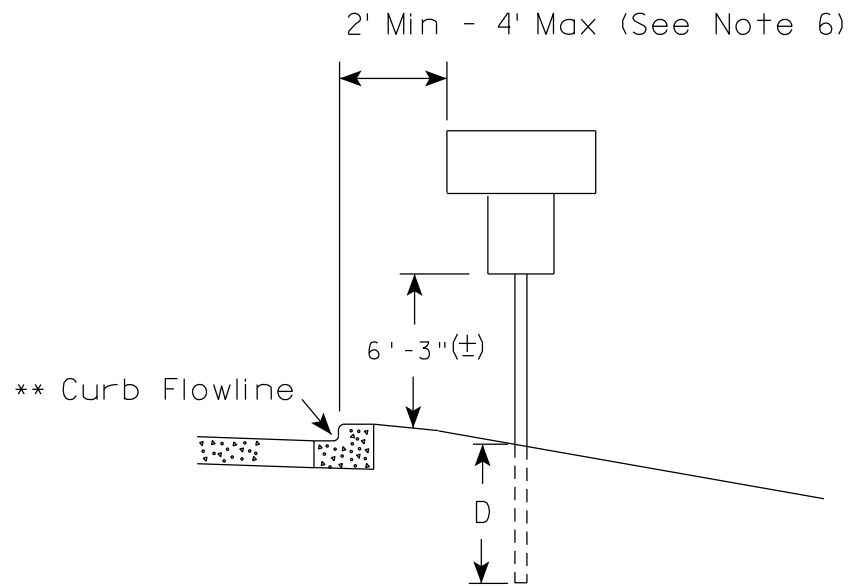
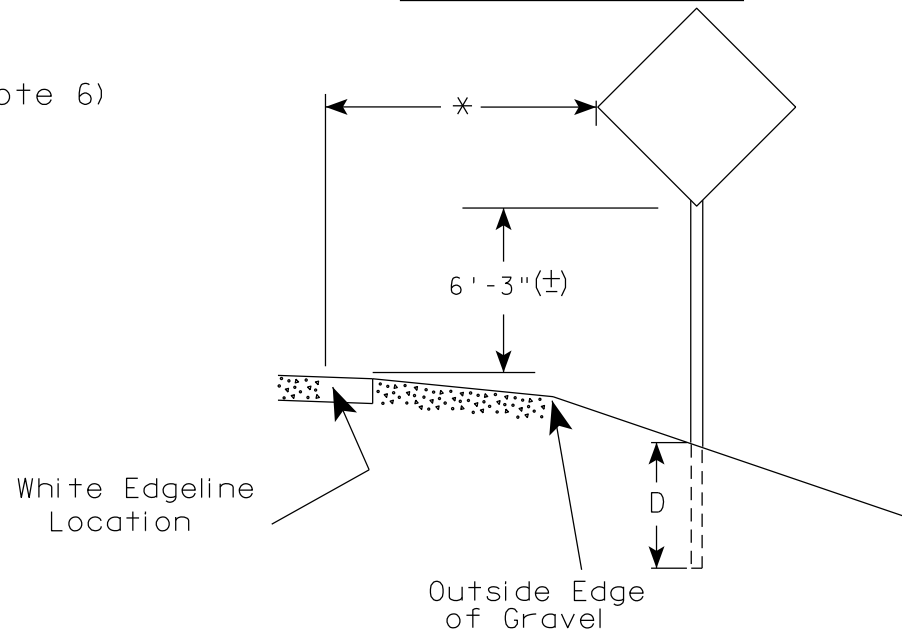
APPROVED
July 2018 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA

URBAN AREA



RURAL AREA (See Note 2)



GENERAL NOTES

1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on or behind barrier wall, see A4-10 sign plate.
The Double Arrow sign (W12-1D) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).
3. For expressways and freeways, mounting height is 7'- 3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
4. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±).
5. Offset distance shall be consistent with existing signs or consistent throughout length of project.
6. The (±) tolerance for mounting height is 3 inches.
7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the Engineer.

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

* * The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

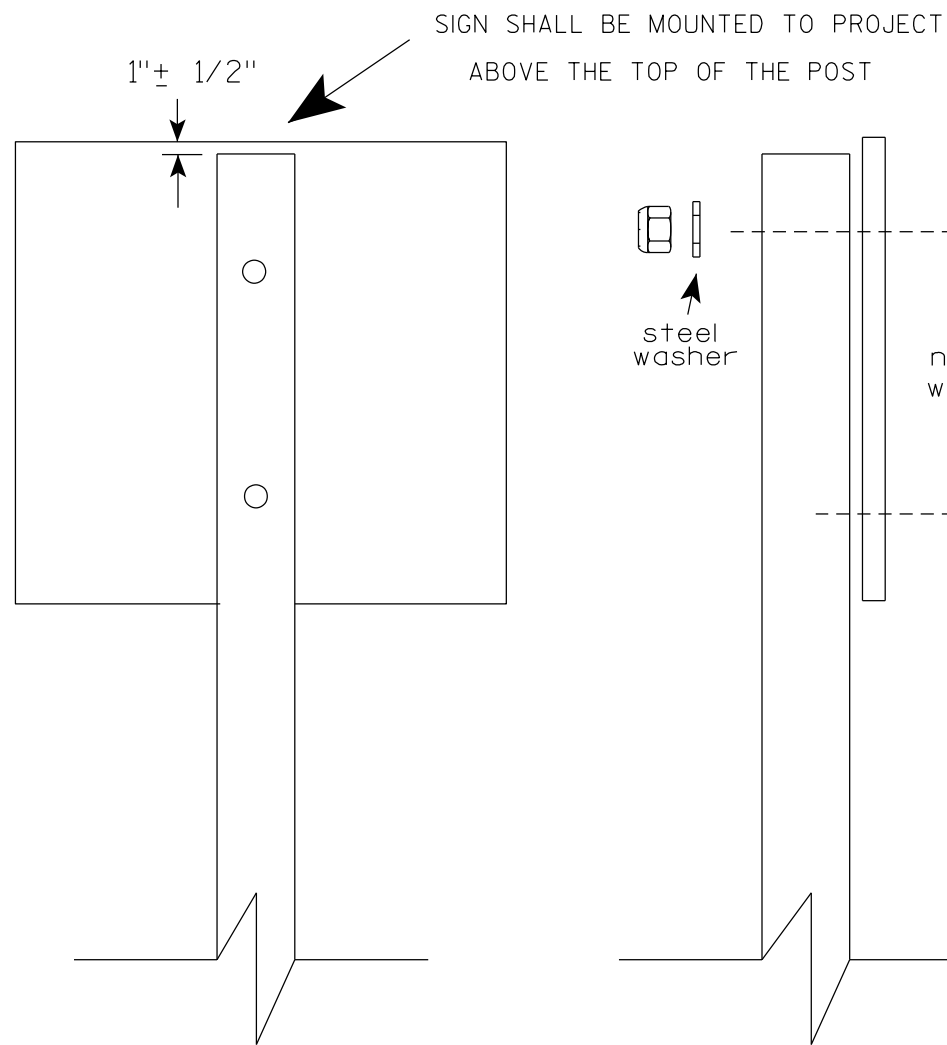
* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 5/13/2020 PLATE NO. A4-3.22



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

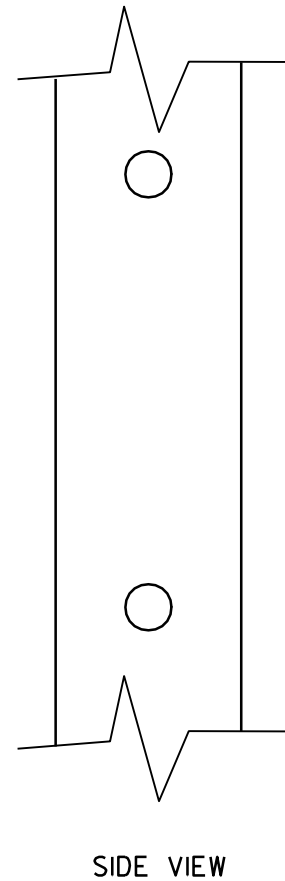
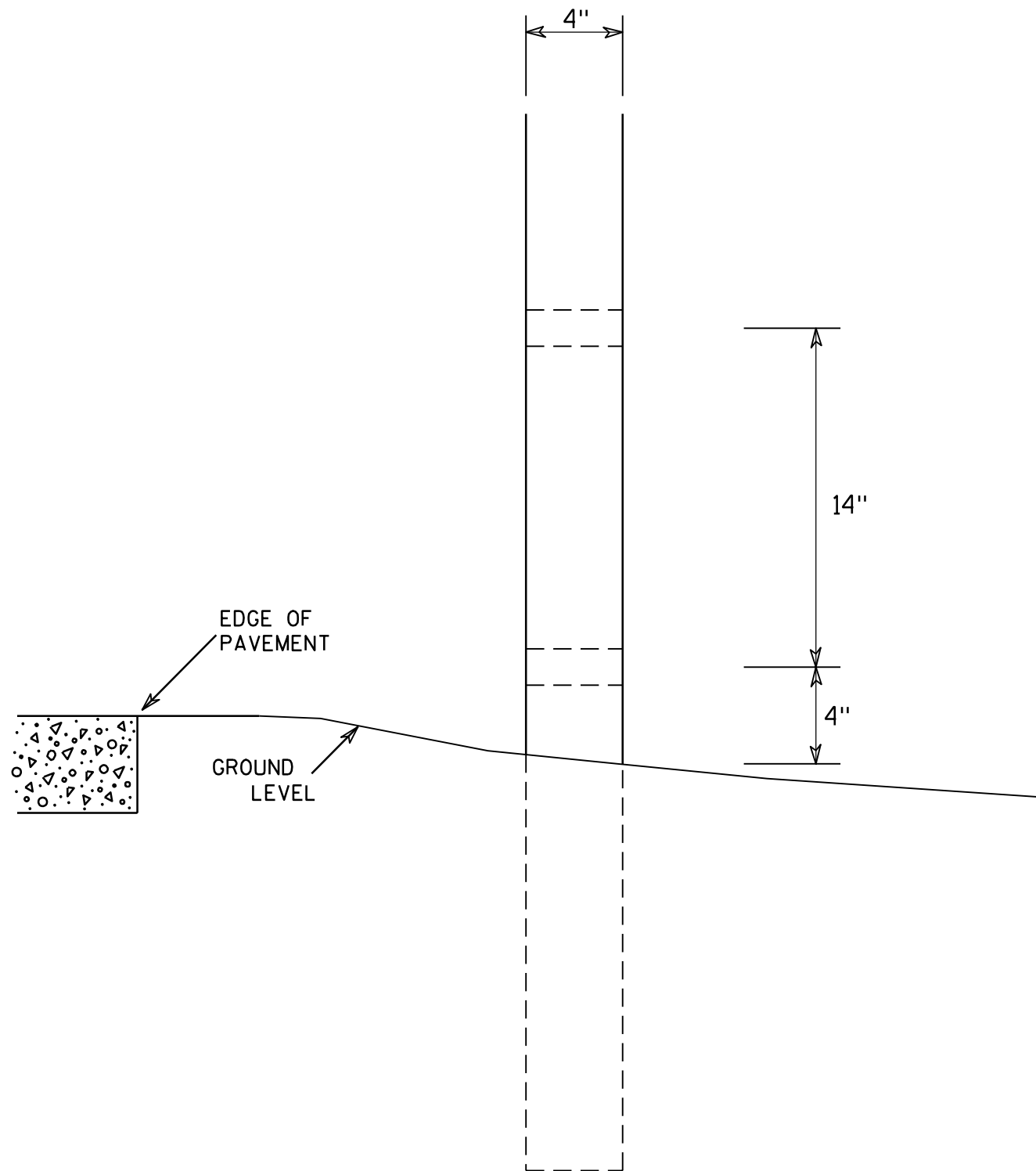
- Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
- Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

- STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)
- MACHINE BOLTS - $\frac{5}{16}$ " X 1-3/4" Length w/ lock nuts
- WOOD POSTS (4" x 6")
- LAG SCREWS - $\frac{3}{8}$ " X 3" (NO STRINGERS ON BACK OF SIGN)
 - $\frac{3}{8}$ " X 4" (STRINGERS ON BACK OF SIGN)
- SQUARE STEEL POSTS (2" x 2")
- MACHINE BOLTS - $\frac{3}{8}$ " X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)
 - $\frac{3}{8}$ " X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)
- RIVETS - $\frac{9}{32}$ " (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
- O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH
- WASHERS (ALL POSTS) -
- 1-1/4" O.D. X $\frac{3}{8}$ " I.D. X $\frac{1}{16}$ " STEEL
 - 1-1/4" O.D. X $\frac{3}{8}$ " I.D. X .080 NYLON

* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS TO POSTS	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R Rauch</i> For State Traffic Engineer
DATE 4/1/2020	PLATE NO. A4-8.9



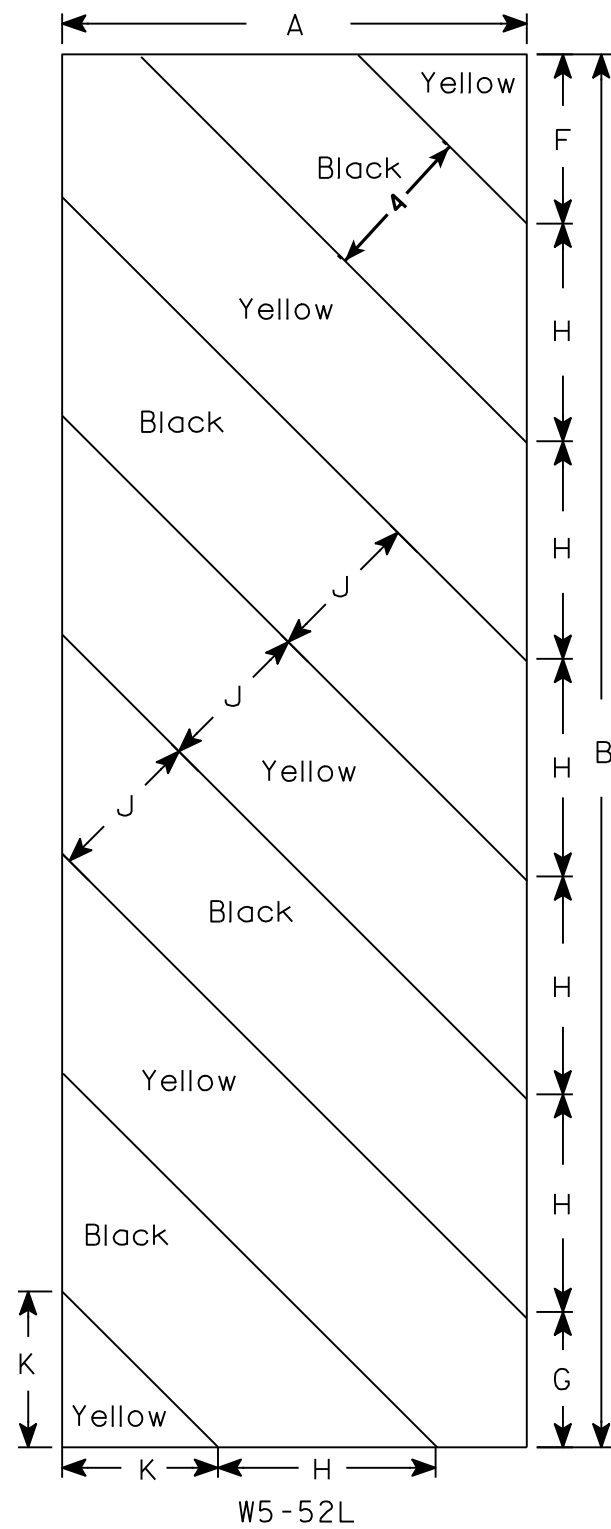
GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

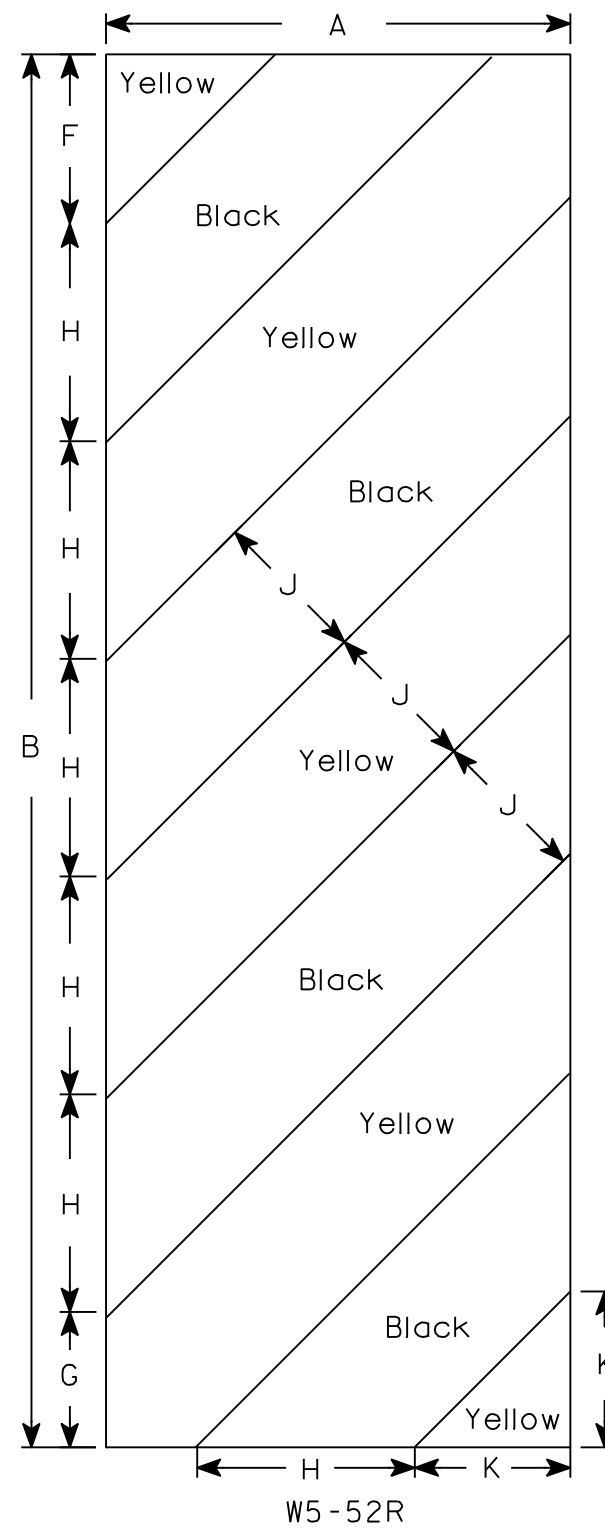
7

7

4 X 6 WOOD POST MODIFICATIONS	
<i>WISCONSIN DEPT OF TRANSPORTATION</i>	
APPROVED	<i>Chester J Spang</i> for State Traffic Engineer
DATE <u>3/27/97</u>	PLATE NO. <u>A4-11.2</u>



W5-52L



W5-52R

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. Alternate colors of stripes as shown.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	12	36				4 3/8	3 1/2	5 5/8	45°	4	4																3.0
2M	12	36				4 3/8	3 1/2	5 5/8	45°	4	4																3.0
3	18	54				6	5 1/2	8 1/2	45°	6	6 9/16																6.75
4																											
5																											

STANDARD SIGN
W5-52L & W5-52R

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 5/29/12 PLATE NO. W5-52.9

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E

DESIGN DATA

LIVE LOAD:

DESIGN LOADING: HL-93
 INVENTORY RATING FACTOR: RF = 1.02
 OPERATING RATING FACTOR: RF = 1.33
 WISCONSIN STANDARD PERMIT VEHICLE (WIS.-SPV): 250 (KIPS)

STRUCTURE IS DESIGNED FOR A FUTURE WEARING SURFACE OF 20 POUNDS PER SQUARE FOOT.

MATERIAL PROPERTIES:

CONCRETE MASONRY:
 SUPERSTRUCTURE _____ $f'_c = 4,000$ P.S.I.
 ALL OTHER _____ $f'_c = 3,500$ P.S.I.
 BAR STEEL REINFORCEMENT:
 GRADE 60 _____ $f_y = 60,000$ P.S.I.

FOUNDATION DATA

ABUTMENTS TO BE SUPPORTED ON HP 10 x 42 PILING DRIVEN SEATED IN PRE-BORED HOLES CORED 3 FEET MINIMUM INTO SOUND BEDROCK. PILE DRIVING IS NOT REQUIRED. THE FACTORED AXIAL RESISTANCE OF PILES IN COMPRESSION USED FOR DESIGN IS 180 TONS MULTIPLIED BY A RESISTANCE FACTOR OF 0.5. ESTIMATED 20 FEET LONG AT W. ABUT. AND 14 FEET AT E. ABUT.

TRAFFIC VOLUME

KINGTON ROAD
 ADT = 40 (2043)
 R.D.S. = 35 M.P.H.

HYDRAULIC DATA

100 YEAR FREQUENCY

$Q_{100} = 700$ C.F.S.
 VEL. = 5.00 F.P.S.
 $HW_{100} = EL. 1258.83$
 WATERWAY AREA = 140.0 SQ. FT.
 DRAINAGE AREA = 3.76 SQ. MI.
 ROADWAY OVERTOPPING = NA
 SCOUR CRITICAL CODE = 5

2 YEAR FREQUENCY

$Q_2 = 190$ C.F.S.
 VEL. = 1.70 F.P.S.
 $HW_2 = EL. 1256.99$

LIST OF DRAWINGS

1. GENERAL PLAN
2. CROSS SECTION & QUANTITIES
3. SUBSURFACE EXPLORATION
4. WEST ABUTMENT
5. WEST ABUTMENT DETAILS
6. EAST ABUTMENT
7. EAST ABUTMENT DETAILS
8. SUPERSTRUCTURE
9. SUPERSTRUCTURE DETAILS
10. TUBULAR STEEL RAILING TYPE 'M'

STRUCTURE DESIGN CONTACTS:

MATT BUCKLI 608-443-0441
 AARON BONK 608-261-0261

THESE PLANS ARE BASED UPON STANDARD BRIDGE PLANS DEVELOPED AND MAINTAINED BY THE WISCONSIN DEPARTMENT OF TRANSPORTATION THROUGH THE USE OF THE WISDOT STANDARD BRIDGE DESIGN TOOL. THE UNDERSIGNED DESIGNER CERTIFIES THE ACCURACY OF THE BRIDGE TYPE, SIZE AND LOCATION, HYDRAULICS AND FOUNDATION SUPPORT, AND INFORMATION IN THE PLANS THAT IS NOT PART OF THE STANDARD PLANS SUPPLIED BY THE DEPARTMENT. THE DESIGNER FURTHER CERTIFIES THAT USE OF THE STANDARD BRIDGE DESIGN TOOL FOR DEVELOPMENT OF THIS PLAN IS CONSISTENT WITH THE GUIDANCE PROVIDED IN THE WISDOT BRIDGE MANUAL.

NO.	DATE	REVISION	BY

Mead & Hunt

Mead & Hunt, Inc.
 2440 Deming Way
 Middleton, WI 53562
 608.273.6380
 www.meadhunt.com

STATE OF WISCONSIN
 DEPARTMENT OF TRANSPORTATION

ACCEPTED *[Signature]* SDR **02/12/24**
 CHIEF STRUCTURES DESIGN ENGINEER DATE

STRUCTURE B-10-0255

KINGTON ROAD OVER NELSON CREEK

COUNTY CLARK TOWN BEAVER

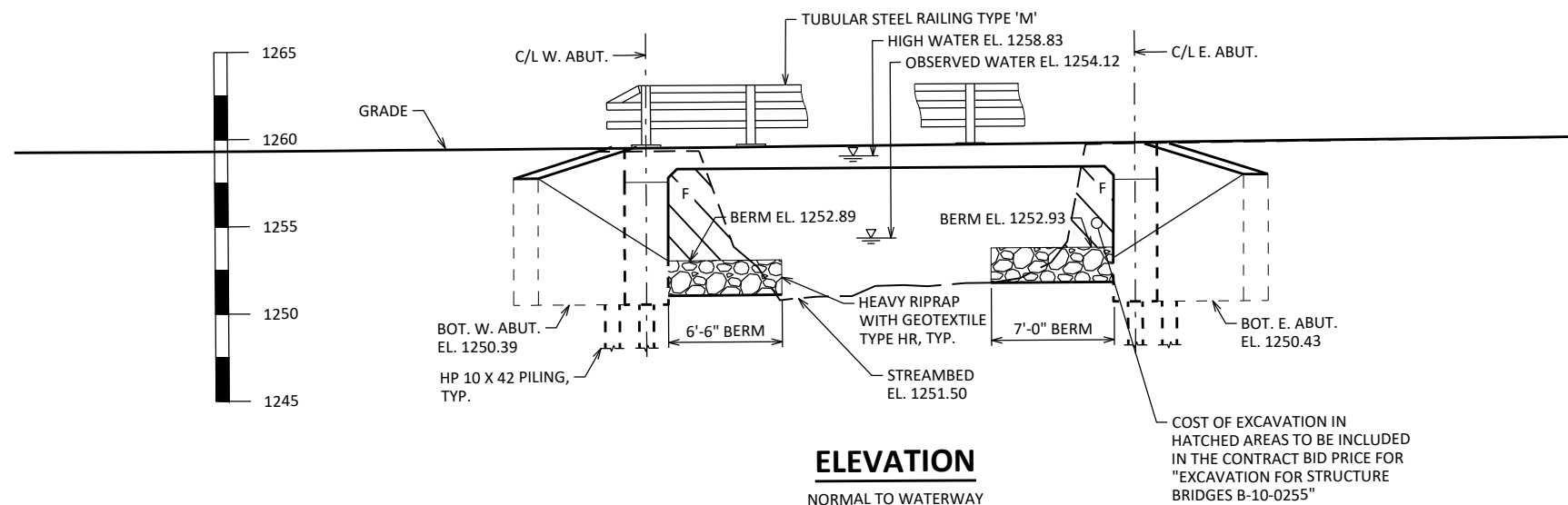
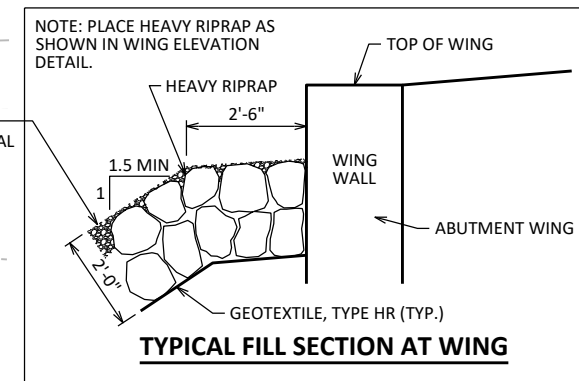
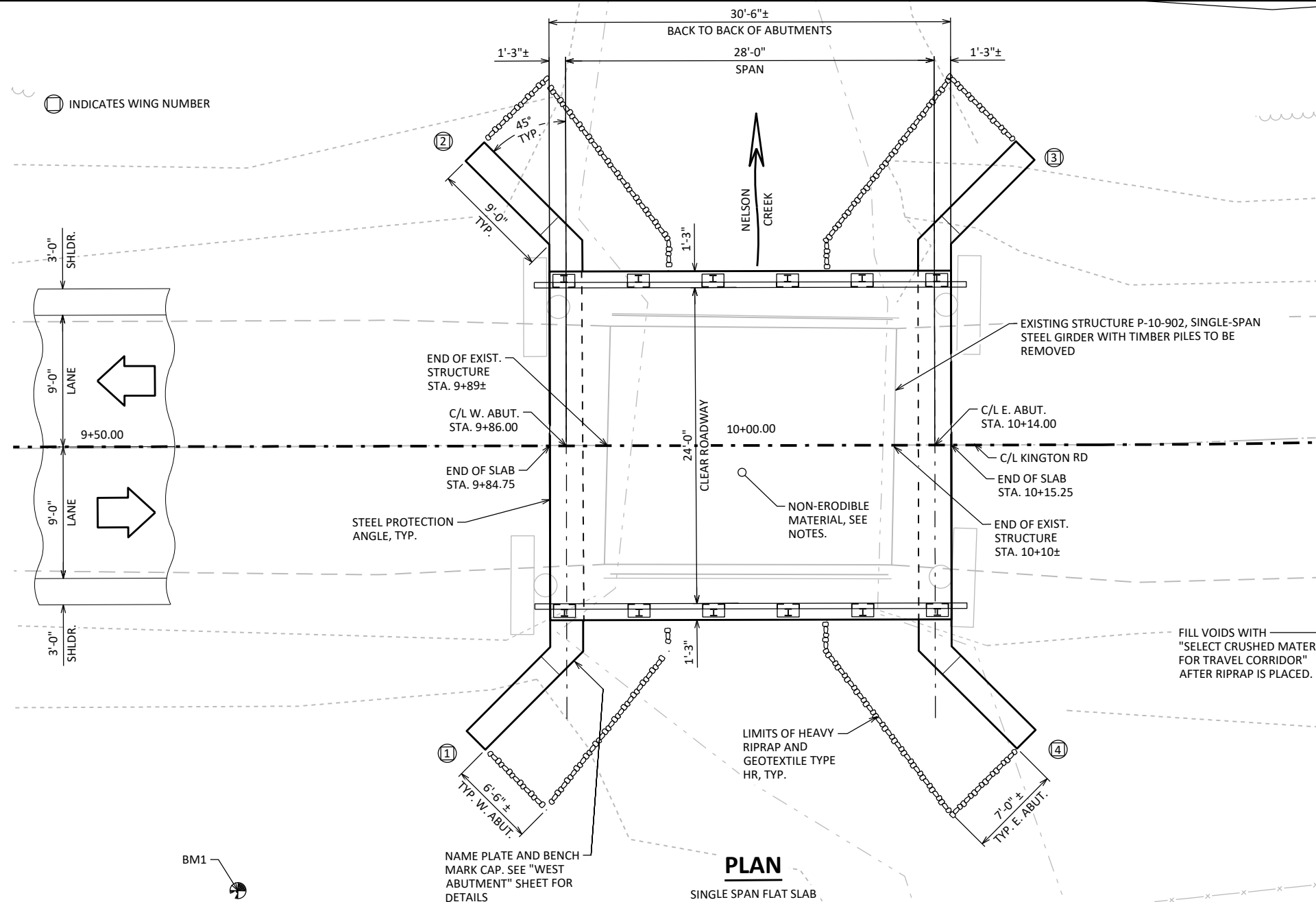
DESIGN SPEC. AASHTO LRFD BRIDGE DESIGN SPECIFICATION

DESIGNED BY TJR	DESIGNED CK'D MJB	DRAWN BY DCV	PLANS CK'D TJR
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GENERAL PLAN SHEET 1 OF 10

I.D. DATE:

INDICATES WING NUMBER

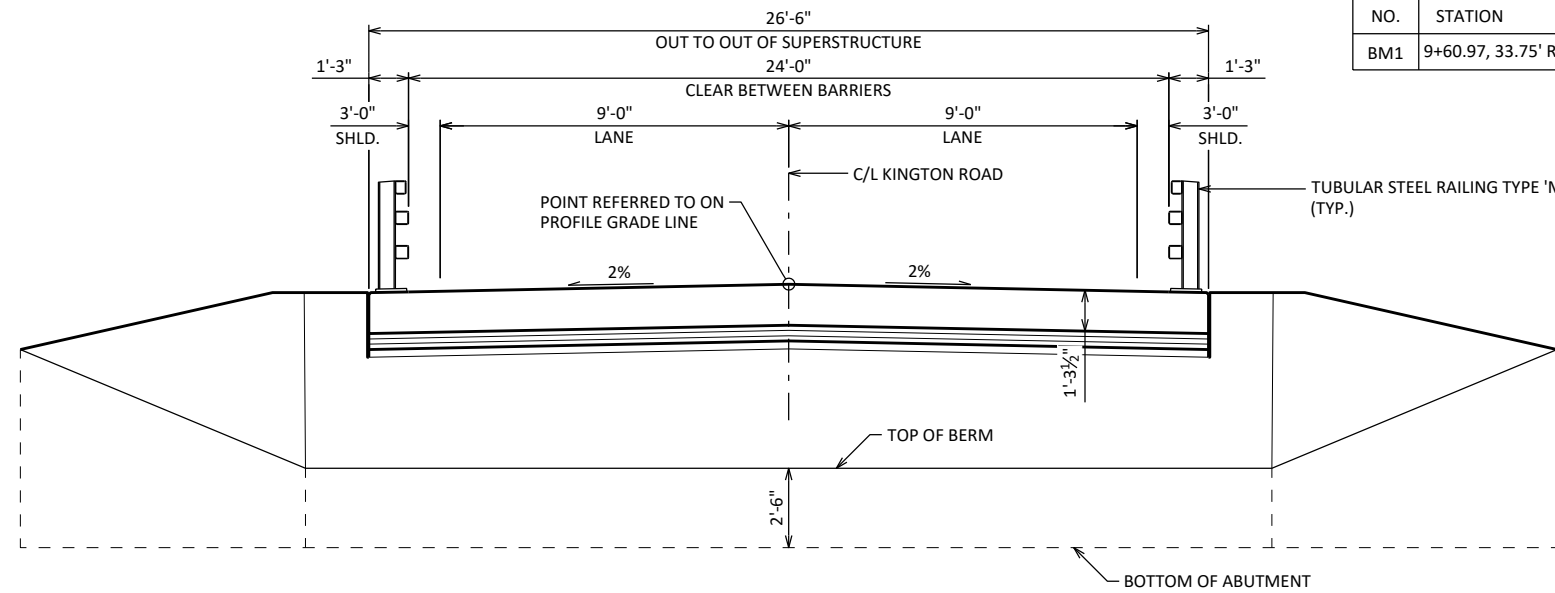


BENCH MARK

NO.	STATION	DESCRIPTION	ELEV.
BM1	9+60.97, 33.75' RT	NAIL IN TREE	1258.05

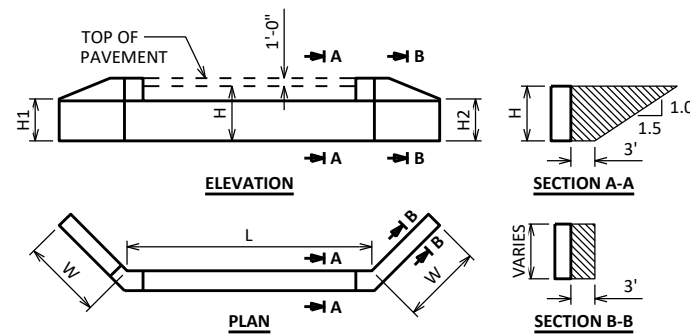
GENERAL NOTES

- DRAWINGS SHALL NOT BE SCALED.
- BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2" CLEAR UNLESS OTHERWISE SHOWN OR NOTED.
- THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE.
- BEVEL EXPOSED EDGES OF CONCRETE 3/4" UNLESS OTHERWISE NOTED.
- THE UPPER LIMITS OF "EXCAVATION FOR STRUCTURES B-10-0255" SHALL BE THE EXISTING GROUNDLINE.
- AT THE BACK FACE OF ABUTMENT ALL VOLUME WHICH CANNOT BE PLACED BEFORE ABUTMENT CONSTRUCTION AND IS NOT OCCUPIED BY THE NEW STRUCTURE SHALL BE BACKFILLED WITH STRUCTURE BACKFILL TYPE A.
- EXCAVATION BELOW THE ABUTMENT AND ABUTMENT BEDDING MATERIALS REQUIRES ENGINEER APPROVAL. GEOTEXTILE SHALL BE SET AT THE BOTTOM OF EXCAVATION AND EXTEND 2'-0" ABOVE BOTTOM OF ABUTMENT.
- THE QUANTITY FOR BACKFILL STRUCTURE IS CALCULATED BASED ON THE DETAIL SHOWN IN THE PLANS.
- THE SLOPE OF THE FILL IN FRONT OF THE ABUTMENTS SHALL BE COVERED WITH HEAVY RIPRAP AND GEOTEXTILE TYPE HR TO THE EXTENT SHOWN ON SHEET 1 AND THE ABUTMENT DETAILS.
- AT ABUTMENTS, CONCRETE POURED UNDER WATER WILL BE ALLOWED AND SHALL BE DONE IN ACCORDANCE WITH SECTION 502.3.5.3 OF THE STANDARD SPECIFICATIONS.
- SLAB FALSEWORK SHALL BE SUPPORTED ON PILES OR THE SUBSTRUCTURE UNLESS AN ALTERNATE METHOD IS APPROVED BY THE ENGINEER.
- PROTECTIVE SURFACE TREATMENT TO BE APPLIED TO ENTIRE EXPOSED TOP OF SLAB, INCLUDING THE SLAB EDGE AND 1'-0" UNDER THE SLAB, THE TOP AND EXTERIOR EXPOSED FACE OF WINGS AND FRONT FACE OF ABUTMENT TO 1'-0" PAST THE EDGE OF SLAB.
- AN APPROXIMATELY 2' BY 4' NON-ERODABLE MATERIAL (ROCK OR CONCRETE) IS LOCATED IN THE STREAM UNDER THE EXISTING STRUCTURE. THE CONTRACTOR SHALL REMOVE THIS MATERIAL TO 1 FT BELOW THE PROPOSED STREAM BED ELEVATION. REMOVAL OF THE NON-ERODABLE MATERIAL SHALL BE INCIDENTAL TO "EXCAVATION FOR STRUCTURES BRIDGES B-10-0255." MATERIAL REMOVED BELOW THE PROPOSED STREAM BED ELEVATION SHALL BE BACKFILLED WITH MATERIAL SATISFACTORY TO THE ENGINEER.
- ALL SURFACES OF RIPRAP HEAVY BOTH ABOVE AND BELOW THE ORDINARY WATERLINE SHALL BE COVERED WITH THE "SELECT CRUSHED MATERIAL FOR TRAVEL CORRIDOR".



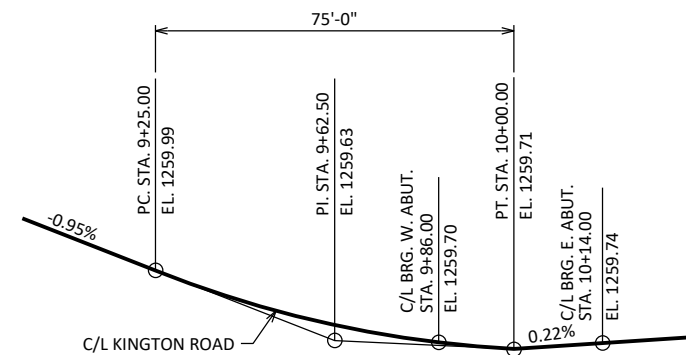
CROSS SECTION THRU ROADWAY

LOOKING UPSTATION
(PILING NOT SHOWN FOR CLARITY)



ABUTMENT BACKFILL DIAGRAM

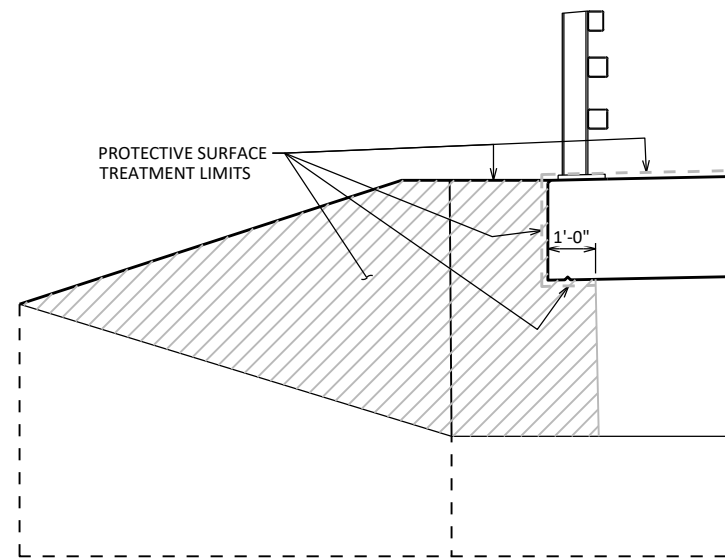
- L = ABUTMENT BODY LENGTH AT BACKFACE (FT)
- H = AVERAGE ABUTMENT FILL HEIGHT (FT)
- H1 = WING 1 HEIGHT AT TIP (FT)
- H2 = WING 2 HEIGHT AT TIP (FT)
- W = WING LENGTH (FT)
- EF = EXPANSION FACTOR (1.20 FOR CY BID ITEMS AND 1.00 FOR TON BID ITEMS)
- $V_{CF} = (L)(3.0')(H) + (L)(0.5)(1.5H)(H) + (3')(0.5)(H1+H2+H+H)(W)$
- $V_{CY} = V_{CF}(EF)/27$
- $V_{TON} = V_{CY}(2.0)$



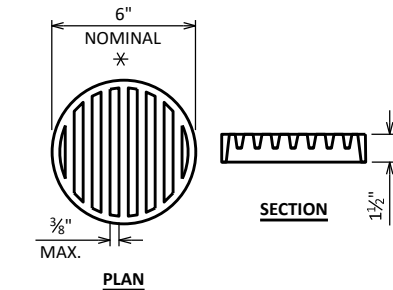
PROFILE GRADE LINE

TOTAL ESTIMATED QUANTITIES

BID ITEM NUMBER	BID ITEMS	UNIT	SUPER	WEST ABUT.	EAST ABUT.	TOTALS
203.0270	REMOVING STRUCTURE OVER WATERWAY DEBRIS CAPTURE P-10-902	EACH	---	---	---	1
206.1001	EXCAVATION FOR STRUCTURES BRIDGES B-10-0255	EACH	---	---	---	1
210.1500	BACKFILL STRUCTURE TYPE A	TON	---	200	200	400
502.0100	CONCRETE MASONRY BRIDGES	CY	43	33	33	109
502.3200	PROTECTIVE SURFACE TREATMENT	SY	103	18	18	139
505.0400	BAR STEEL REINFORCEMENT HS STRUCTURES	LB	---	2,110	2,110	4,220
505.0600	BAR STEEL REINFORCEMENT HS COATED STRUCTURES	LB	9,580	1,540	1,540	12,660
506.0105	STRUCTURAL STEEL CARBON	LB	540	---	---	540
513.4061	RAILING TUBULAR TYPE M	LF	66	---	---	66
516.0500	RUBBERIZED MEMBRANE WATERPROOFING	SY	---	5	5	10
550.0020	PRE-BORING ROCK OR CONSOLIDATED MATERIALS	LF	---	101	59	160
550.1100	PILING STEEL HP 10-INCH X 42 LB	LF	---	130	88	218
606.0300	RIPRAP HEAVY	CY	---	26	29	55
612.0406	PIPE UNDERDRAIN WRAPPED 6-INCH	LF	---	69	69	138
645.0111	GEOTEXTILE TYPE DF SCHEDULE A	SY	---	42	42	84
645.0120	GEOTEXTILE TYPE HR	SY	---	39	44	83
SPV.0195.01	SELECT CRUSHED MATERIAL FOR TRAVEL CORRIDOR	TON	---	15	16	31
	NON-BID ITEMS					
	FILLER	SIZE	---	---	---	1/2", 3/4"

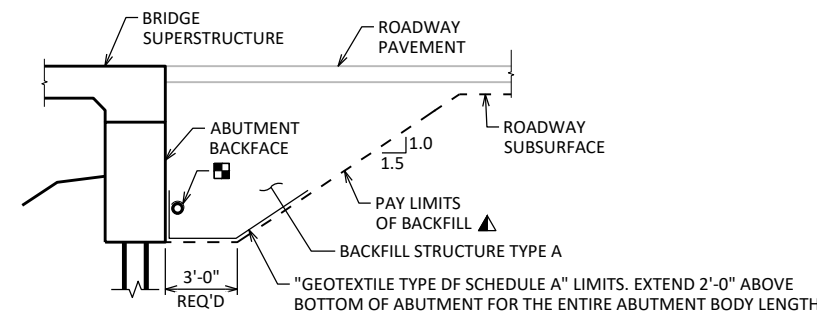


PROTECTIVE SURFACE TREATMENT DETAILS



RODENT SHIELD DETAIL

- * DIMENSIONS ARE APPROXIMATE. THE GRATE IS SIZED TO FIT INTO A PIPE COUPLING. ORIENT SO SLOTS ARE VERTICAL.
- THE RODENT SHIELD, PIPE COUPLING AND SCREWS SHALL BE CONSIDERED INCIDENTAL TO THE BID ITEM "PIPE UNDERDRAIN WRAPPED 6-INCH".
- THE RODENT SHIELD SHALL BE A PVC GRATE SIMILAR TO THIS DETAIL. THE GRATE IS COMMERCIALY AVAILABLE AS A FLOOR STRAINER. A PIPE COUPLING IS REQUIRED FOR THE ATTACHMENT OF THIS SHIELD TO THE EXPOSED END OF THE PIPE UNDERDRAIN. THE SHIELD SHALL BE FASTENED TO THE PIPE COUPLING WITH TWO OR MORE NO. 10 X 1-INCH STAINLESS STEEL SHEET METAL SCREWS.



TYPICAL SECTION THRU ABUTMENT

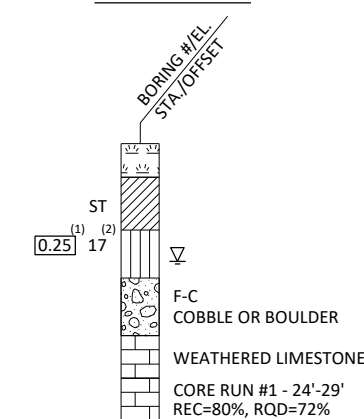
- ▲ BACKFILL PAY LIMITS. BACKFILL BEYOND PAY LIMITS SHALL BE INCIDENTAL TO EXCAVATION FOR STRUCTURES. LIMITS OF EXCAVATION SHALL BE DETERMINED BY THE CONTRACTOR.
- PIPE UNDERDRAIN WRAPPED (6-INCH). SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. ATTACH RODENT SHIELD AT ENDS OF PIPE UNDERDRAIN.

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-10-0255			
DRAWN BY		DCV	PLANS CK'D TJR
CROSS SECTION & QUANTITIES			SHEET 2

MATERIAL SYMBOLS

ASPHALT	TOPSOIL	PEAT
CONCRETE	FILL	GRAVEL
SAND	CLAY	SILT
BOULDERS OR COBBLES	LIMESTONE	BEDROCK (UNKNOWN)
SHALE	SANDSTONE	IGNEOUS/META

LEGEND OF BORING



(1) UNCONFINED STRENGTH, AS DETERMINED BY A POCKET PENETROMETER (TSF)

(2) UNLESS OTHERWISE SPECIFIED THE SPT 'N' VALUE IS BASED ON AASHTO T-206, STANDARD PENETRATION TEST. THE SPT 'N' VALUE PRESENTED HAS NOT BEEN CORRECTED FOR OVERBURDEN PRESSURE OR HAMMER EFFICIENCY.

GROUND WATER ELEVATION

- ▽ AT TIME OF DRILLING
- ▼ END OF DRILLING
- ▽ AFTER DRILLING

ABBREVIATIONS

F-FINE M-MEDIUM C-COARSE ST-SHELBY TUBE

SUBSURFACE EXPLORATION FOR FOUNDATION DESIGN AND BIDDERS INFORMATION

BORINGS WERE COMPLETED AT POINTS APPROXIMATELY AS INDICATED ON THIS DRAWING TO OBTAIN INFORMATION CONCERNING THE CHARACTER OF SUBSURFACE MATERIALS FOUND AT THE SITE. BECAUSE THE INVESTIGATED DEPTHS ARE LIMITED AND THE AREA OF THE BORINGS IS VERY SMALL IN RELATION TO THE ENTIRE SITE, THE WISCONSIN DEPARTMENT OF TRANSPORTATION DOES NOT WARRANT SIMILAR SUBSURFACE CONDITIONS BELOW, BETWEEN, OR BEYOND THESE BORINGS. VARIATIONS IN SOIL CONDITIONS SHOULD BE EXPECTED AND FLUCTUATIONS IN GROUNDWATER LEVELS MAY OCCUR.

NO.	DATE	REVISION	BY
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STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

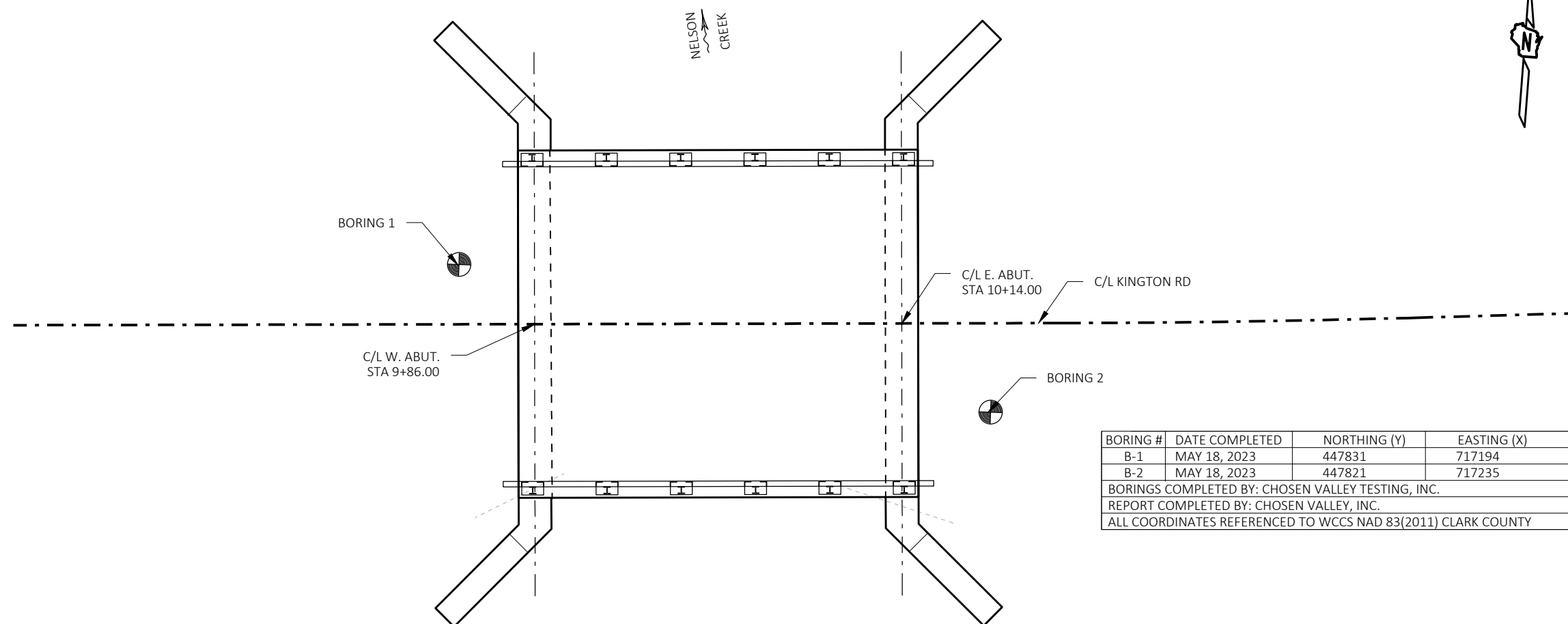
STRUCTURE B-10-0255

DRAWN BY	TJR	PLANS CK'D	MJB
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SUBSURFACE EXPLORATION

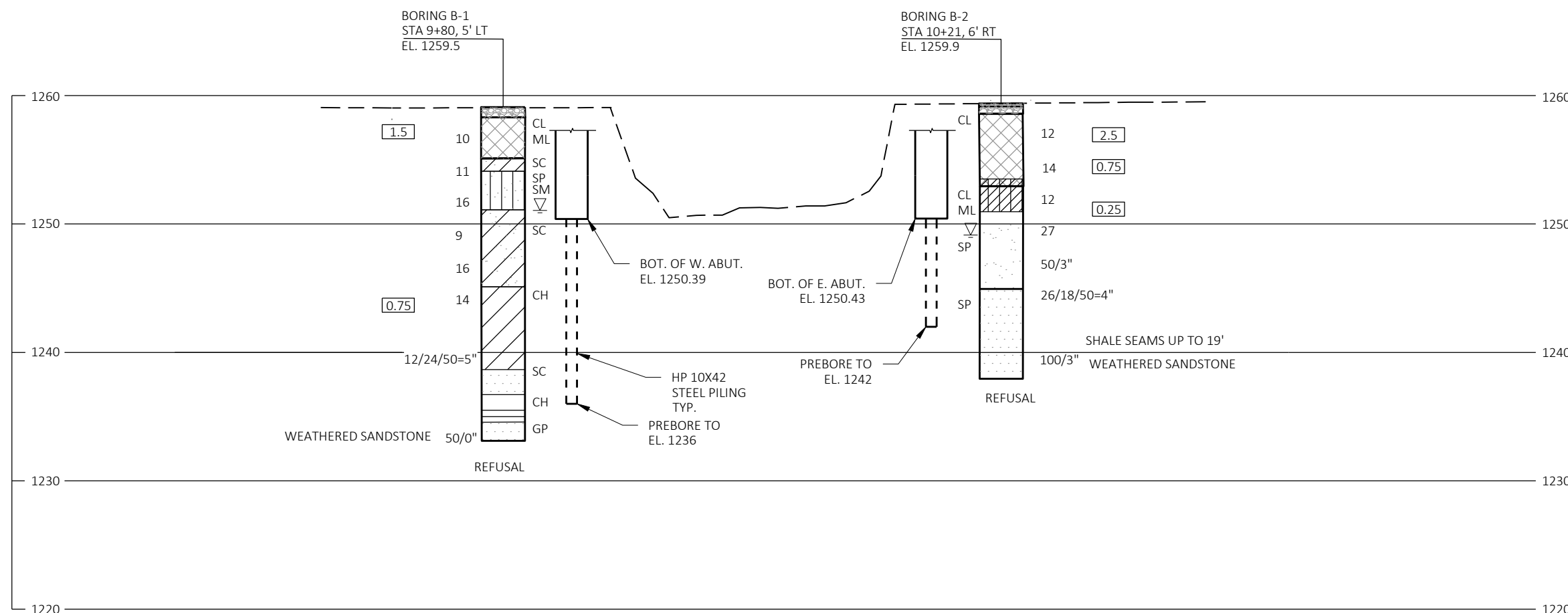
SHEET 3

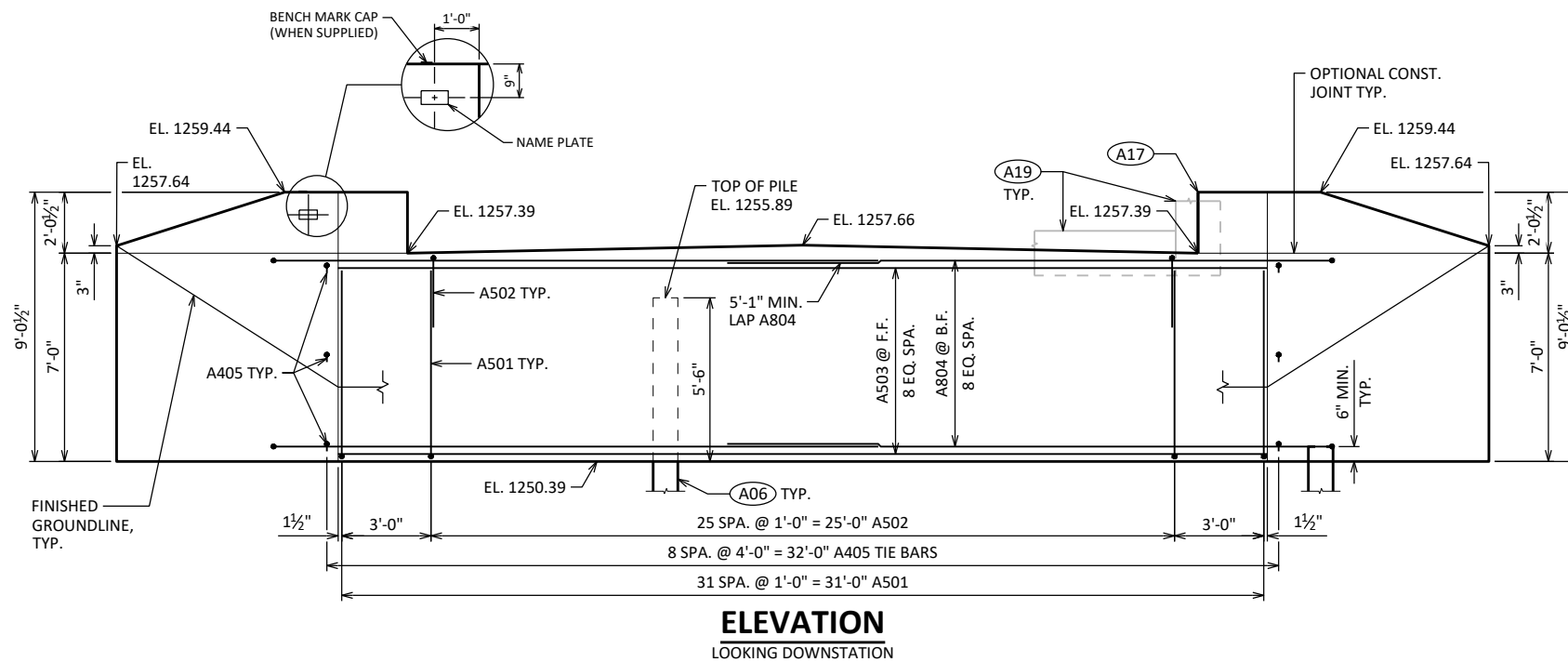
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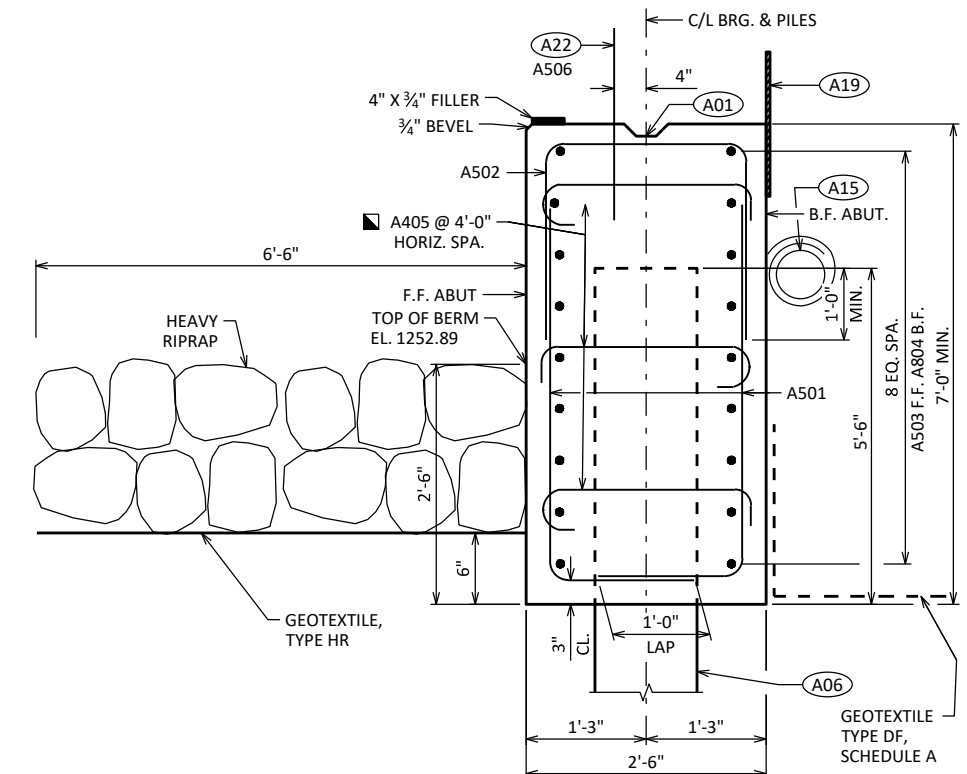
BORING #	DATE COMPLETED	NORTHING (Y)	EASTING (X)
B-1	MAY 18, 2023	447831	717194
B-2	MAY 18, 2023	447821	717235

BORINGS COMPLETED BY: CHOSEN VALLEY TESTING, INC.
REPORT COMPLETED BY: CHOSEN VALLEY, INC.
ALL COORDINATES REFERENCED TO WCCS NAD 83(2011) CLARK COUNTY

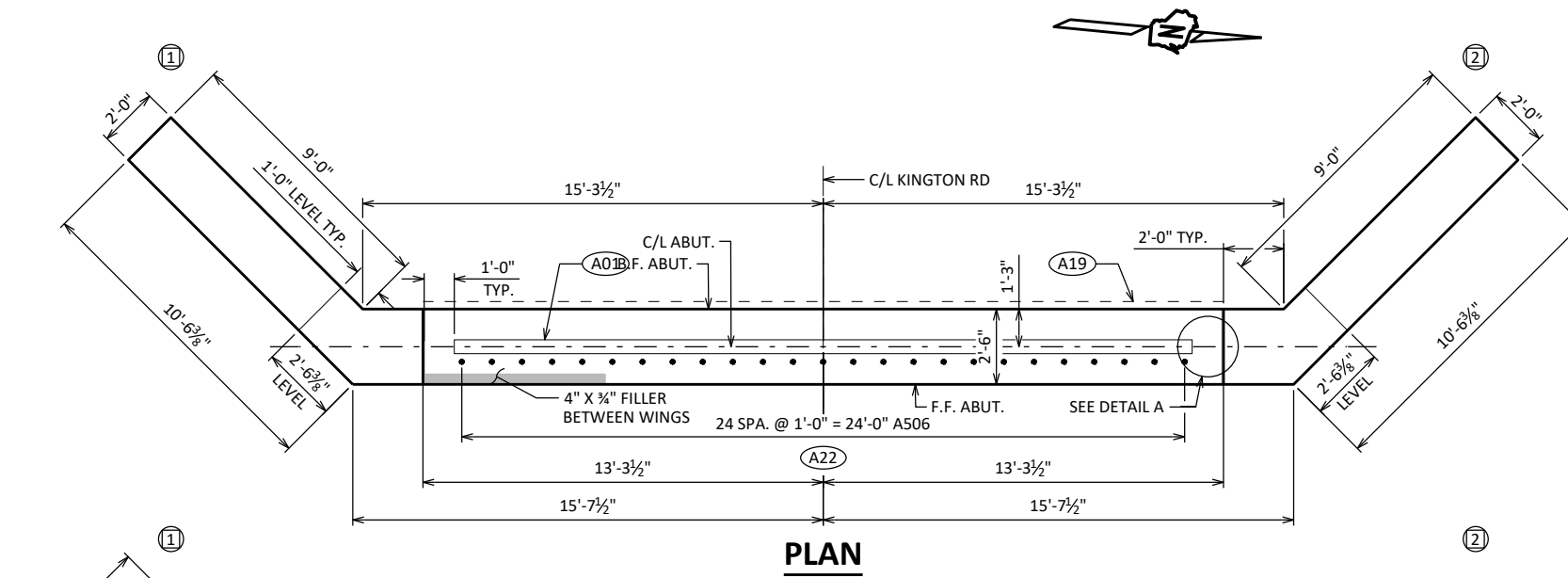




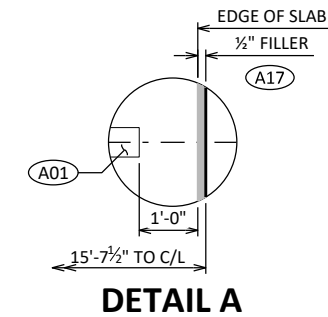
ELEVATION
LOOKING DOWNSTATION



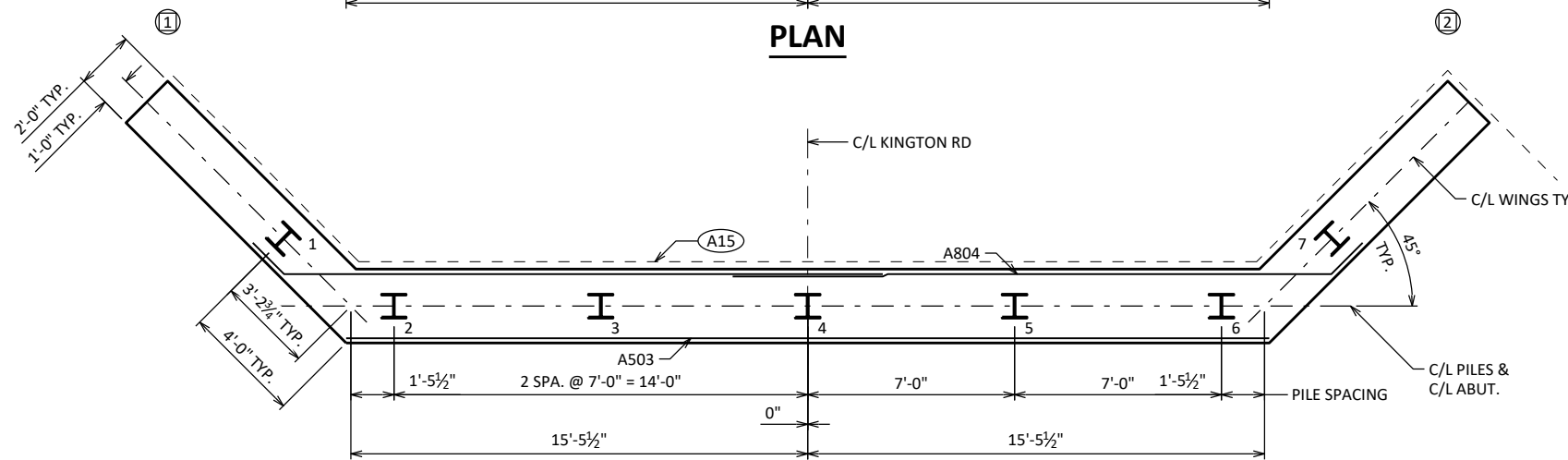
SECTION THRU BODY



PLAN



DETAIL A



PILE PLAN

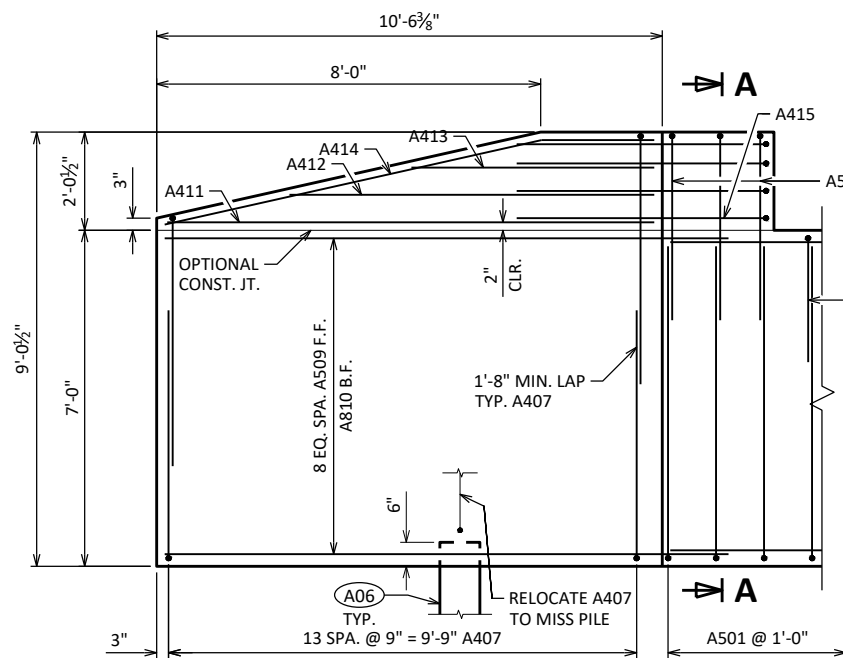
- (A01) CONST. JOINT: KEYWAY FORMED BY A BEVELED 2X6.
 - (A06) SUPPORT ABUTMENT ON HP 10 X 42 STEEL PILING, ESTIMATED 20 FEET LONG SEATED IN PRE-BORED HOLES CORE 3 FEET MINIMUM INTO SOUND BEDROCK. PILE DRIVING IS NOT REQUIRED. THE FACTORED AXIAL RESISTANCE OF PILES IN COMPRESSION USED FOR DESIGN IS 180 TONS MULTIPLIED BY A RESISTANCE FACTOR OF 0.5.
 - (A15) PIPE UNDERDRAIN WRAPPED (6-INCH). SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. RODENT SHIELD REQUIRED. PLACE 5 FEET ABOVE BOTTOM OF ABUTMENT.
 - (A17) 1/2" FILLER: SEAL ALL EXPOSED HORIZ. & VERT. SURFACES OF 1/2" FILLER WITH NON-STAINING GRAY NON-BITUMINOUS JOINT SEALER. (1" DEEP AND HOLD 3/8" BELOW SURFACE OF CONCRETE). EXTEND SEALER 3" BELOW GUTTER LINE AT INSIDE FACE.
 - (A19) 18" RUBBERIZED MEMBRANE WATERPROOFING. SEAL ALL HORIZ. & VERT. JOINTS AT BACKFACE.
 - (A22) A506 BARS SPACED @ 1'-0" CNTRS. MAY BE PLACED AFTER CONCRETE IS POURED BUT BEFORE INITIAL SET HAS TAKEN PLACE. (EMBED 1'-0" INTO CONC.)
- ALTERNATE THE POSITION OF THE 90° AND 180° HOOKS AT EACH VERTICAL LAYER OF TIES.

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-10-0255			
DRAWN BY		DCV	PLANS CK'D TJR
WEST ABUTMENT			SHEET 4

BILL OF BARS

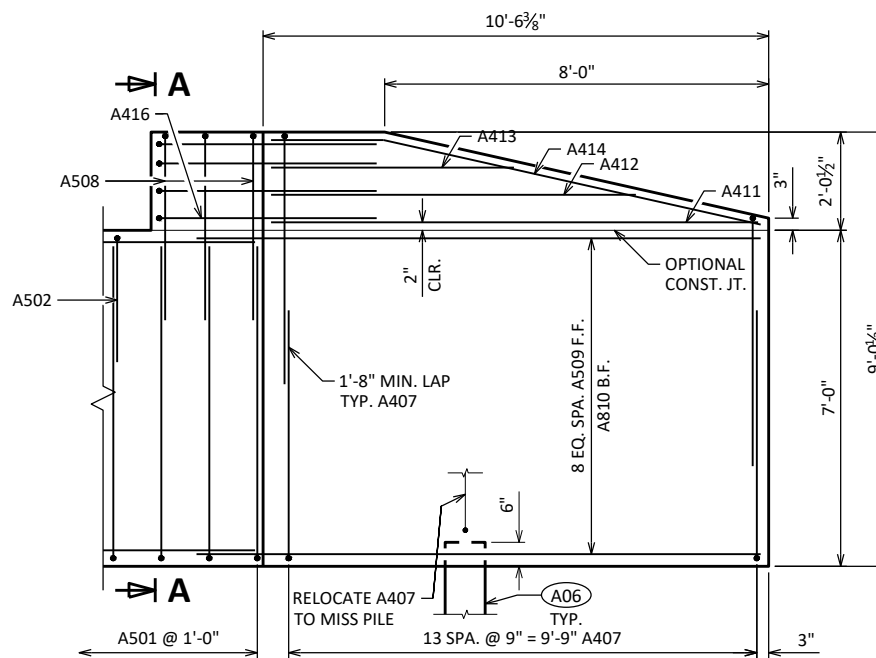
NOTE: THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE.

BAR MARK	COAT	NO. REQ'D.	LENGTH	BENT	BAR SERIES	LOCATION
A501		64	8'-0"	X		ABUT BODY STIRRUPS
A502		26	7'-1"	X		ABUT BODY STIRRUPS - TOP U-BAR
A503		9	31'-3"			ABUT BODY HORIZ. - F.F.
A804		18	21'-7"	X		ABUT BODY HORIZ. - B.F.
A405		27	3'-0"	X		ABUT BODY TIE BARS
A506	X	25	2'-0"			ABUT BODY DOWEL BARS
A407	X	56	11'-10"	X		WING STIRRUPS
A508	X	6	9'-7"	X		WING CORNER STIRRUPS
A509	X	18	11'-9"	X		WING LOWER HORIZ. - F.F.
A810	X	18	13'-3"	X		WING LOWER HORIZ. - B.F.
A411	X	4	10'-0"			WING UPPER HORIZ.
A412	X	4	7'-6"			WING UPPER HORIZ.
A413	X	4	4'-11"			WING UPPER HORIZ.
A414	X	4	9'-7"	X		WING TOP HORIZ.
A415	X	4	8'-3"	X		WING 1 UPPER HORIZ. CORNER
A416	X	4	8'-3"	X		WING 2 UPPER HORIZ. CORNER



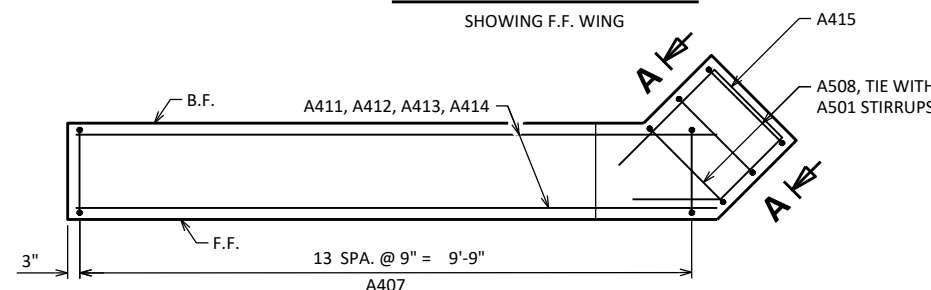
WING 1 ELEVATION

SHOWING F.F. WING



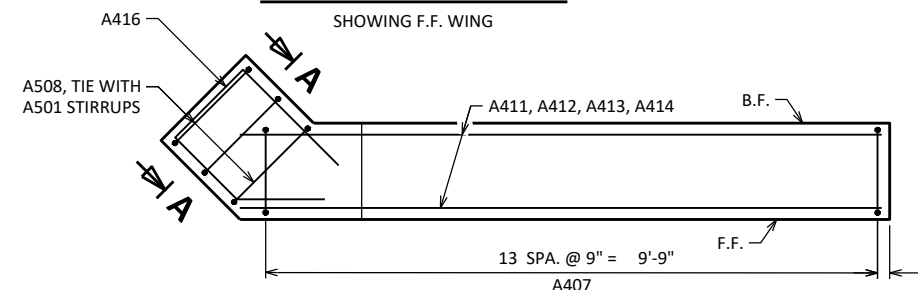
WING 2 ELEVATION

SHOWING F.F. WING



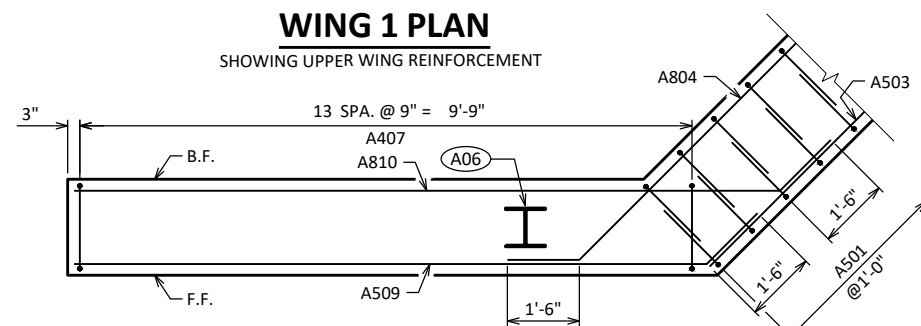
WING 1 PLAN

SHOWING UPPER WING REINFORCEMENT



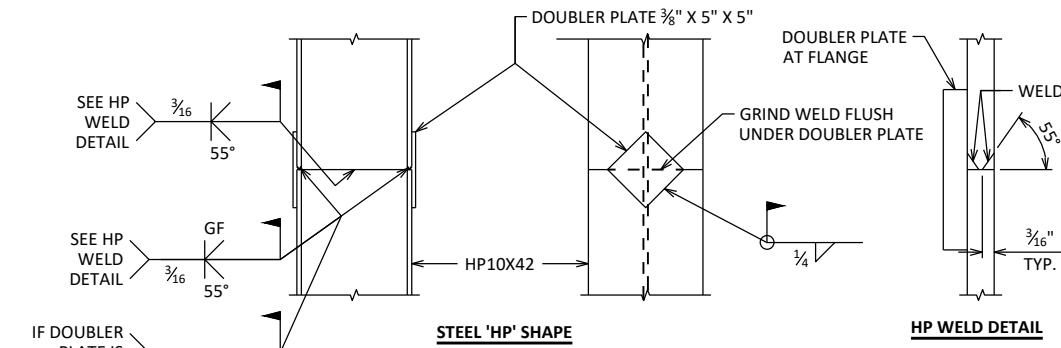
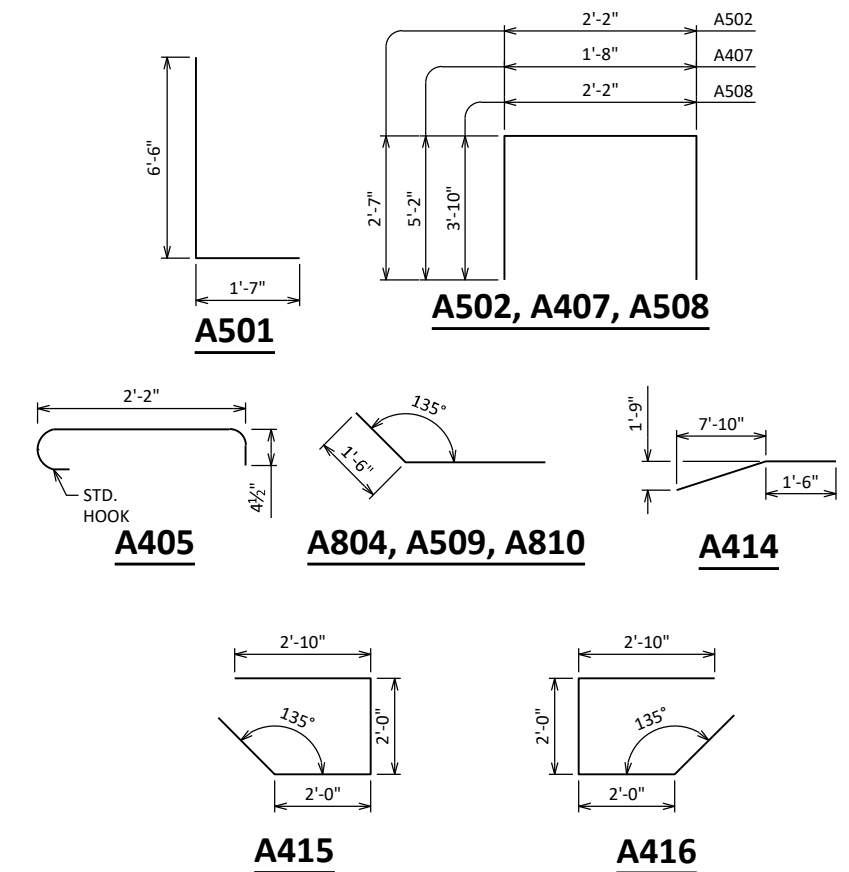
WING 2 PLAN

SHOWING UPPER WING REINFORCEMENT

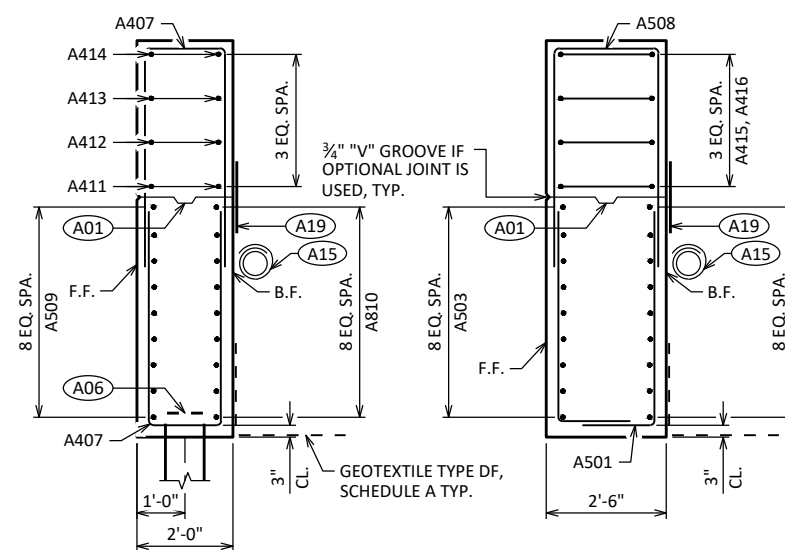


WING 1 PLAN

SHOWING LOWER WING REINFORCEMENT
WING 2 SIMILAR



'HP' PILE DETAILS



SECTION THRU WING 1

TYPICAL BOTH WINGS

SECTION A-A

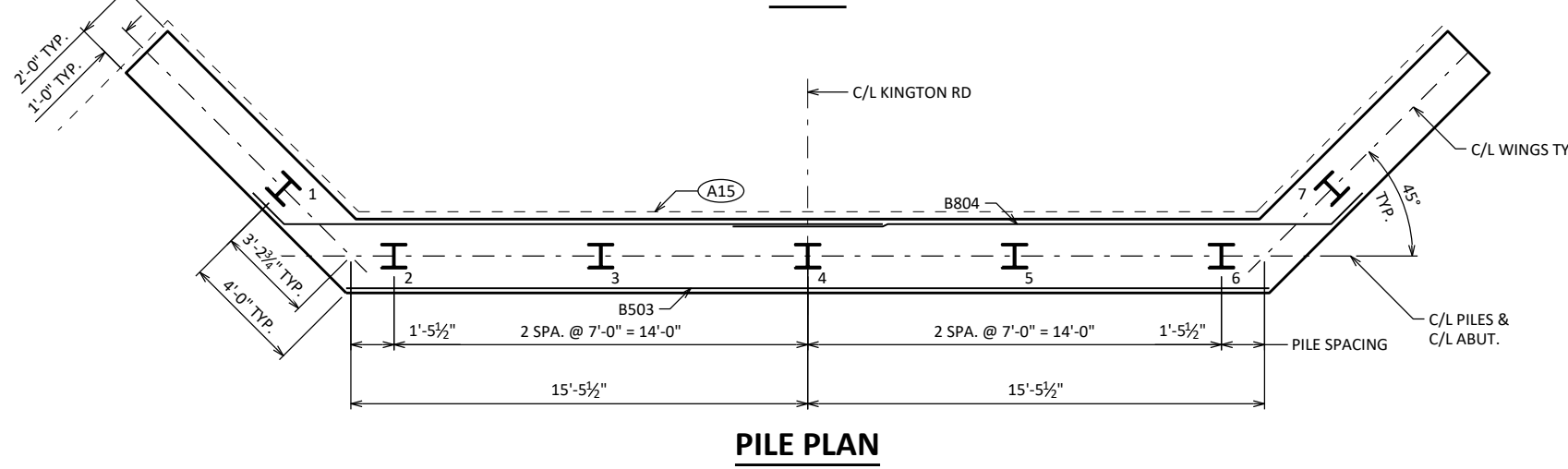
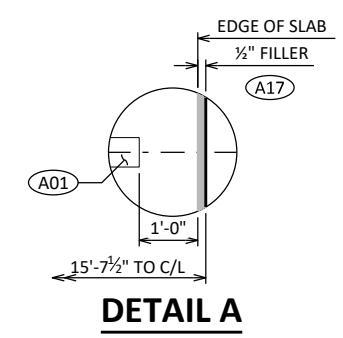
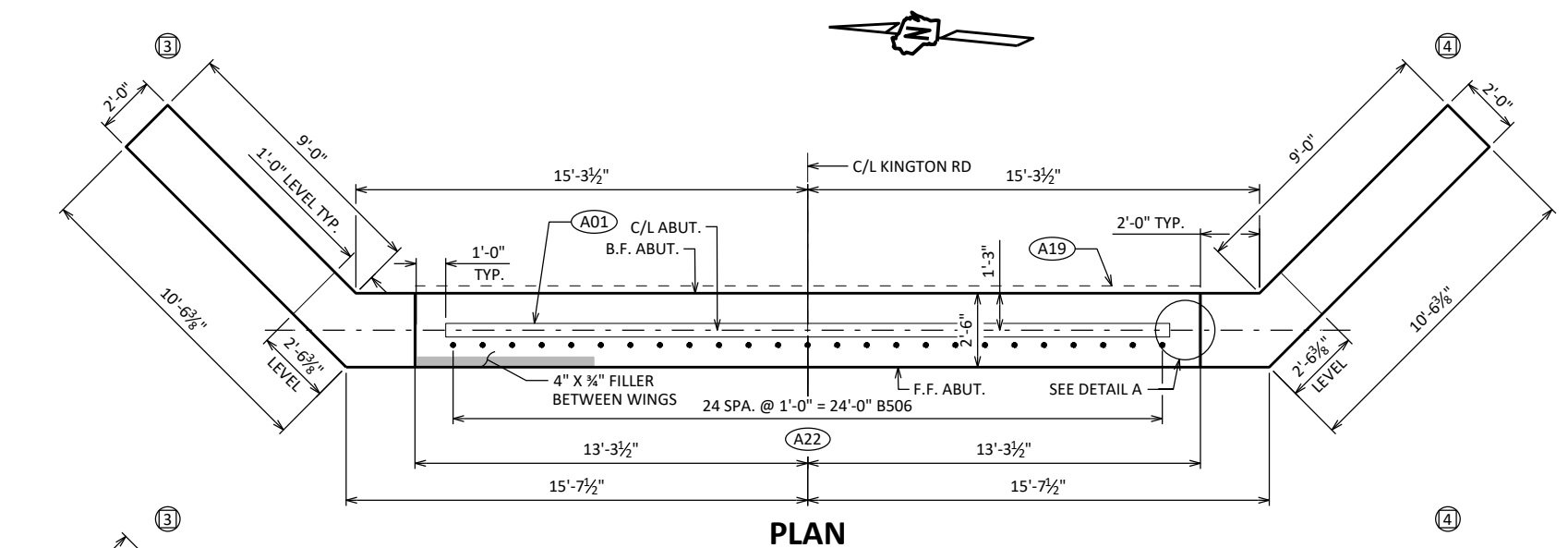
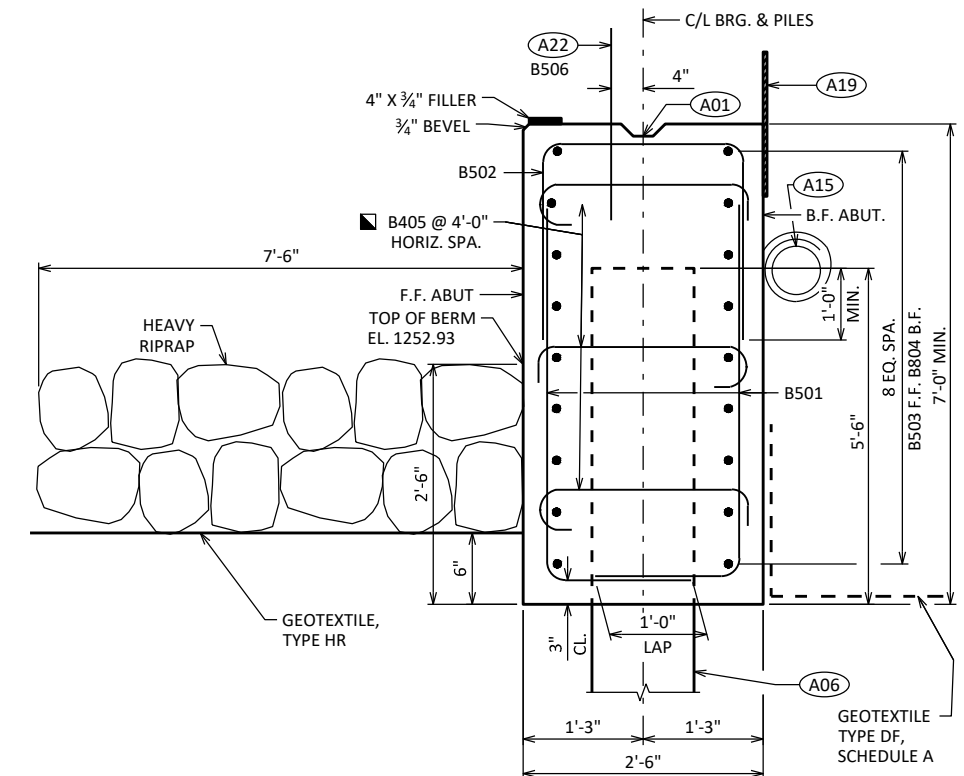
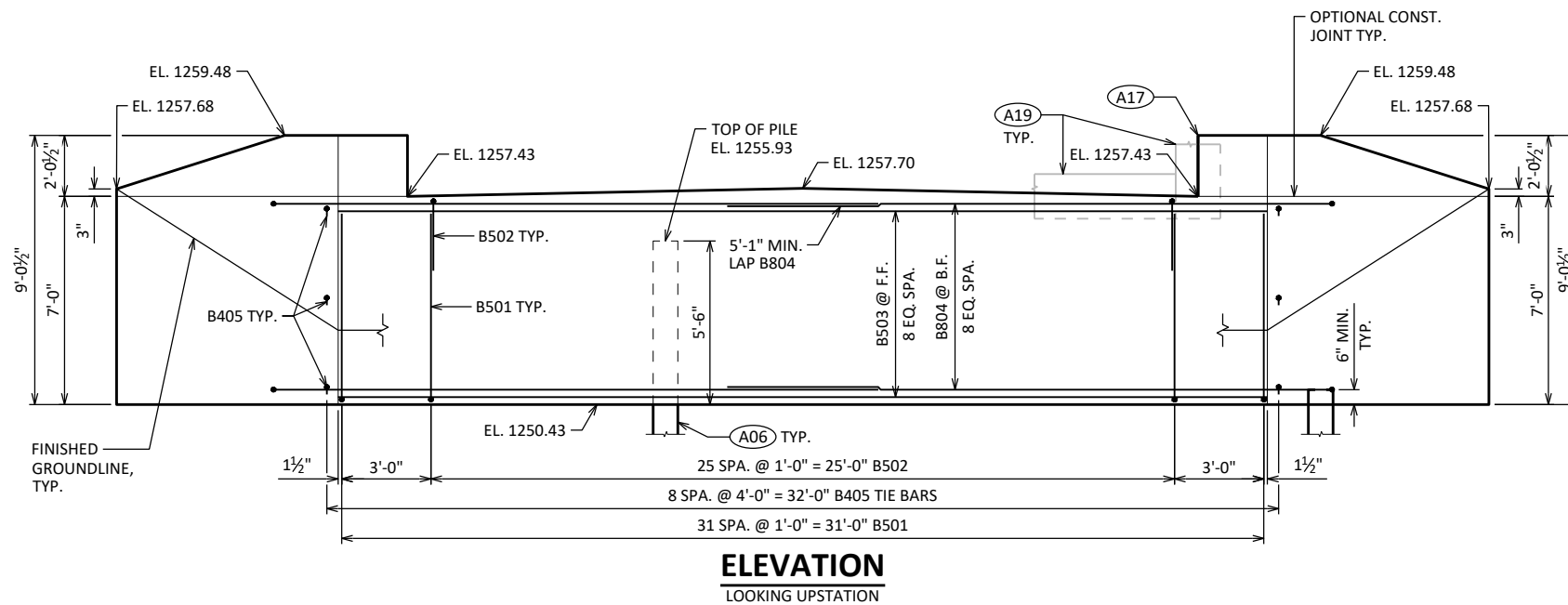
- (A01) OPTIONAL CONST. JOINT: KEYWAY FORMED BY A BEVELED 2X6. PROVIDE 3/4" "V" GROOVE ON F.F. OF WINGWALL IF JOINT IS USED.
- (A06) SUPPORT ABUTMENT ON HP 10 X 42 STEEL PILING, ESTIMATED 15 FEET LONG SEATED IN PRE-BORED HOLES CORE 3 FEET MINIMUM INTO SOUND BEDROCK. PILE DRIVING IS NOT REQUIRED. THE FACTORED AXIAL RESISTANCE OF PILES IN COMPRESSION USED FOR DESIGN IS 180 TONS MULTIPLIED BY A RESISTANCE FACTOR OF 0.5.
- (A15) PIPE UNDERDRAIN WRAPPED (6-INCH). SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. RODENT SHIELD REQUIRED.
- (A19) 18" RUBBERIZED MEMBRANE WATERPROOFING, ONLY IF OPTIONAL CONSTRUCTION JOINT IS USED. COST INCIDENTAL TO BID ITEM "CONCRETE MASONRY STRUCTURES".

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-10-0255			
DRAWN BY		DCV	PLANS CK'D TJR
WEST ABUTMENT DETAILS		SHEET 5	

8

8

SCALE =



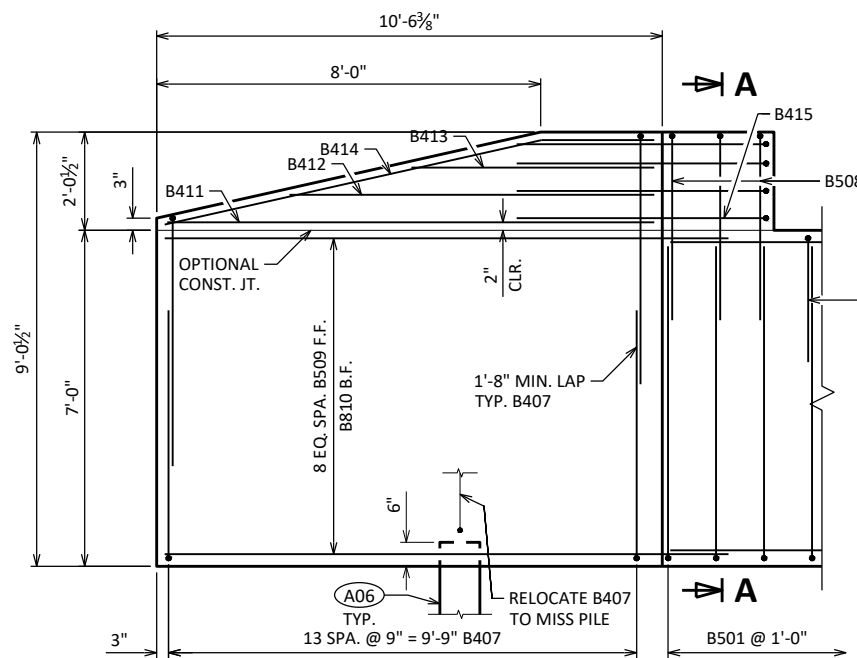
- (A01) CONST. JOINT: KEYWAY FORMED BY A BEVELED 2X6.
 - (A06) SUPPORT ABUTMENT ON HP 10 X 42 STEEL PILING, ESTIMATED 14 FEET LONG SEATED IN PRE-BORED HOLES CORE 3 FEET MINIMUM INTO SOUND BEDROCK. PILE DRIVING IS NOT REQUIRED. THE FACTORED AXIAL RESISTANCE OF PILES IN COMPRESSION USED FOR DESIGN IS 180 TONS MULTIPLIED BY A RESISTANCE FACTOR OF 0.5.
 - (A15) PIPE UNDERDRAIN WRAPPED (6-INCH). SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. RODENT SHIELD REQUIRED. PLACE 5 FEET ABOVE BOTTOM OF ABUTMENT.
 - (A17) 1/2" FILLER: SEAL ALL EXPOSED HORIZ. & VERT. SURFACES OF 1/2" FILLER WITH NON-STAINING GRAY NON-BITUMINOUS JOINT SEALER. (1" DEEP AND HOLD 3/8" BELOW SURFACE OF CONCRETE). EXTEND SEALER 3" BELOW GUTTER LINE AT INSIDE FACE.
 - (A19) 18" RUBBERIZED MEMBRANE WATERPROOFING. SEAL ALL HORIZ. & VERT. JOINTS AT BACKFACE.
 - (A22) B506 BARS SPACED @ 1'-0" CNTRS. MAY BE PLACED AFTER CONCRETE IS POURED BUT BEFORE INITIAL SET HAS TAKEN PLACE. (EMBED 1'-0" INTO CONC.)
- ALTERNATE THE POSITION OF THE 90° AND 180° HOOKS AT EACH VERTICAL LAYER OF TIES.

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-10-0255			
DRAWN BY		DCV	PLANS CK'D TJR
EAST ABUTMENT			SHEET 6

BILL OF BARS

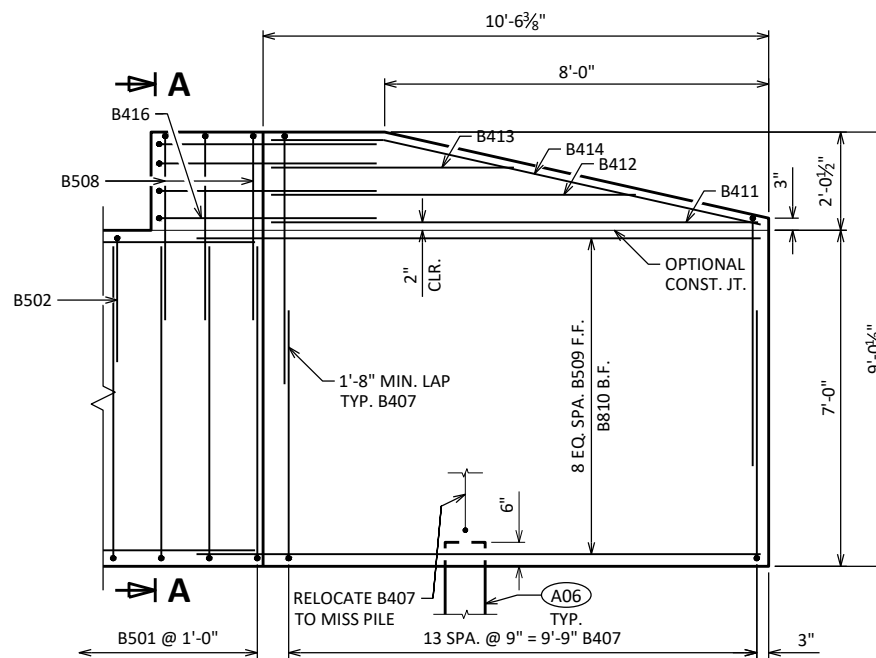
NOTE: THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE.

BAR MARK	COAT	NO. REQ'D.	LENGTH	BENT	BAR SERIES	LOCATION
B501		64	8'-0"	X		ABUT BODY STIRRUPS
B502		26	7'-1"	X		ABUT BODY STIRRUPS - TOP U-BAR
B503		9	31'-3"			ABUT BODY HORIZ. - F.F.
B804		18	21'-7"	X		ABUT BODY HORIZ. - B.F.
B405		27	3'-0"	X		ABUT BODY TIE BARS
B506	X	25	2'-0"			ABUT BODY DOWEL BARS
B407	X	56	11'-10"	X		WING STIRRUPS
B508	X	6	9'-7"	X		WING CORNER STIRRUPS
B509	X	18	11'-9"	X		WING LOWER HORIZ. - F.F.
B810	X	18	13'-3"	X		WING LOWER HORIZ. - B.F.
B411	X	4	10'-0"			WING UPPER HORIZ.
B412	X	4	7'-6"			WING UPPER HORIZ.
B413	X	4	4'-11"			WING UPPER HORIZ.
B414	X	4	9'-7"	X		WING TOP HORIZ.
B415	X	4	8'-3"	X		WING 3 UPPER HORIZ. CORNER
B416	X	4	8'-3"	X		WING 4 UPPER HORIZ. CORNER



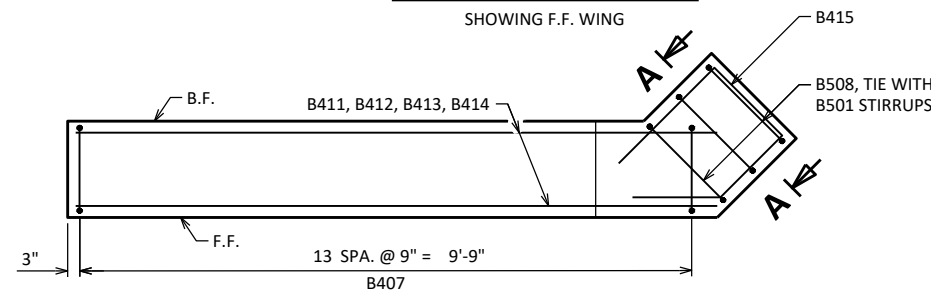
WING 3 ELEVATION

SHOWING F.F. WING



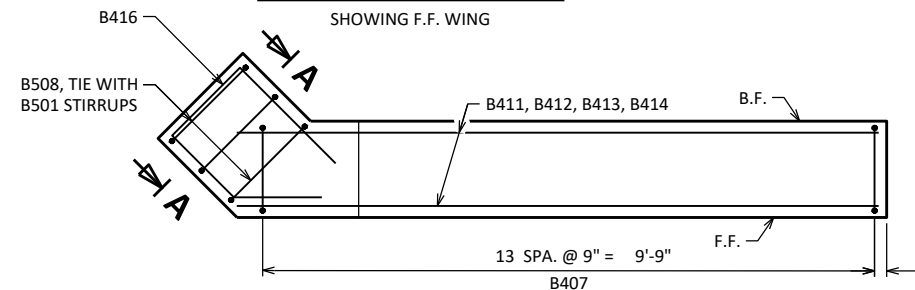
WING 4 ELEVATION

SHOWING F.F. WING



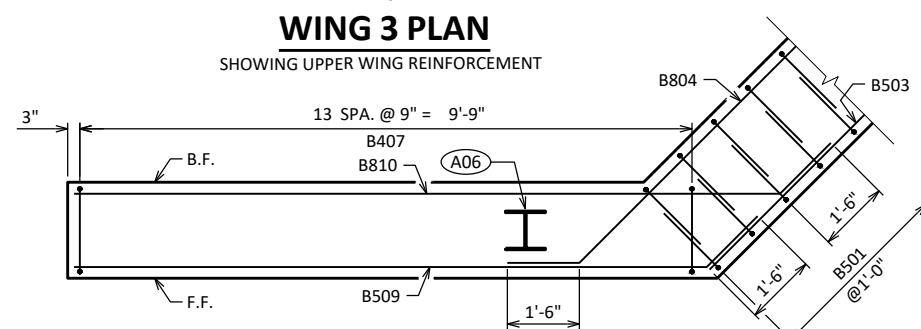
WING 3 PLAN

SHOWING UPPER WING REINFORCEMENT



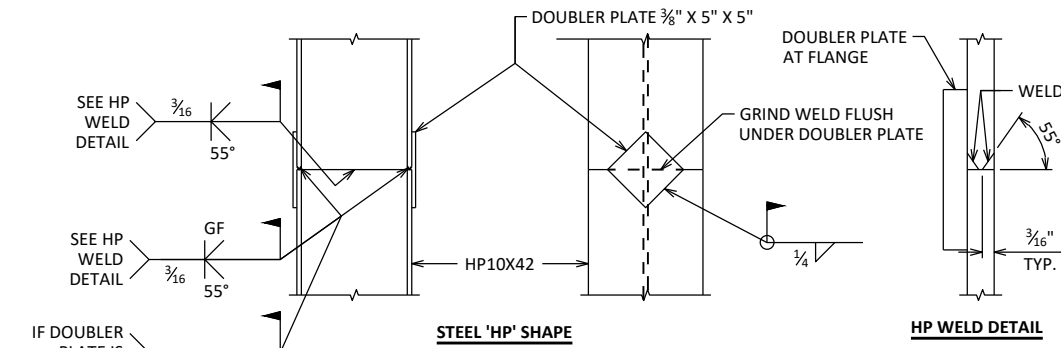
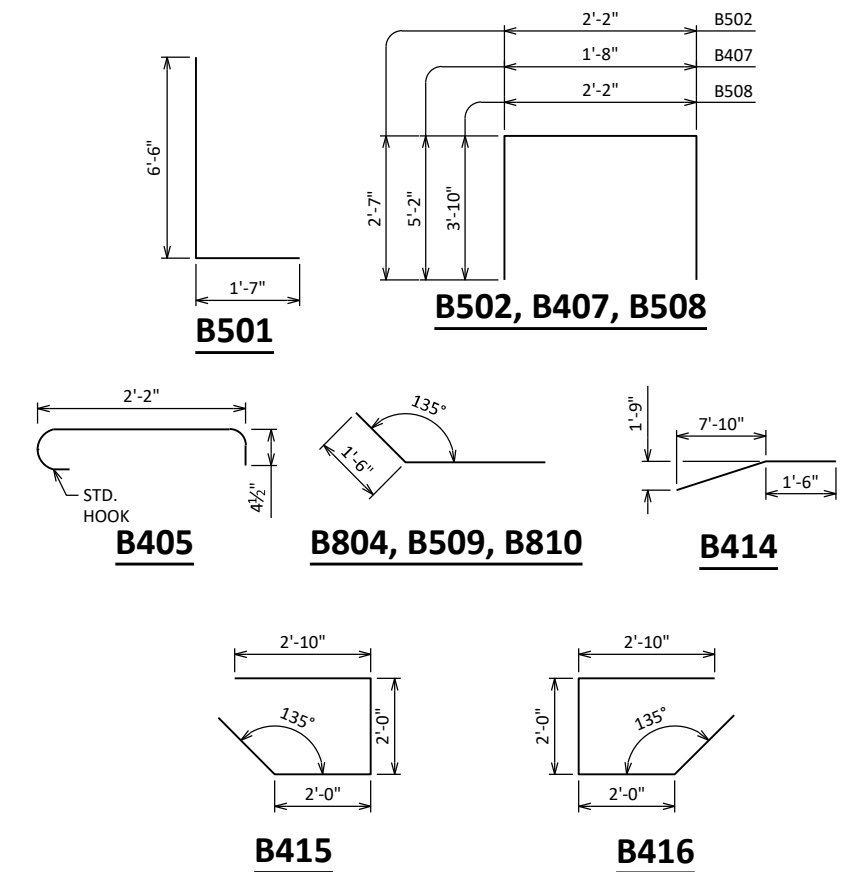
WING 4 PLAN

SHOWING UPPER WING REINFORCEMENT

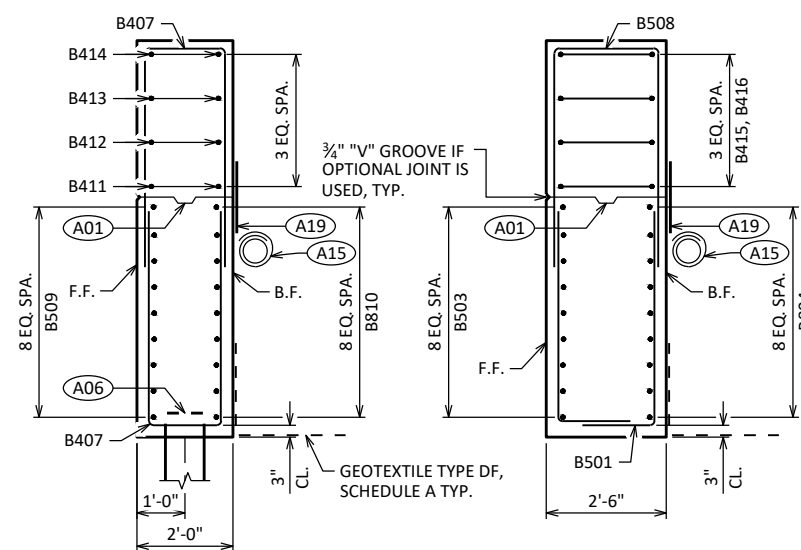


WING 3 PLAN

SHOWING LOWER WING REINFORCEMENT
WING 4 SIMILAR



'HP' PILE DETAILS



SECTION THRU WING 3

TYPICAL BOTH WINGS

SECTION A-A

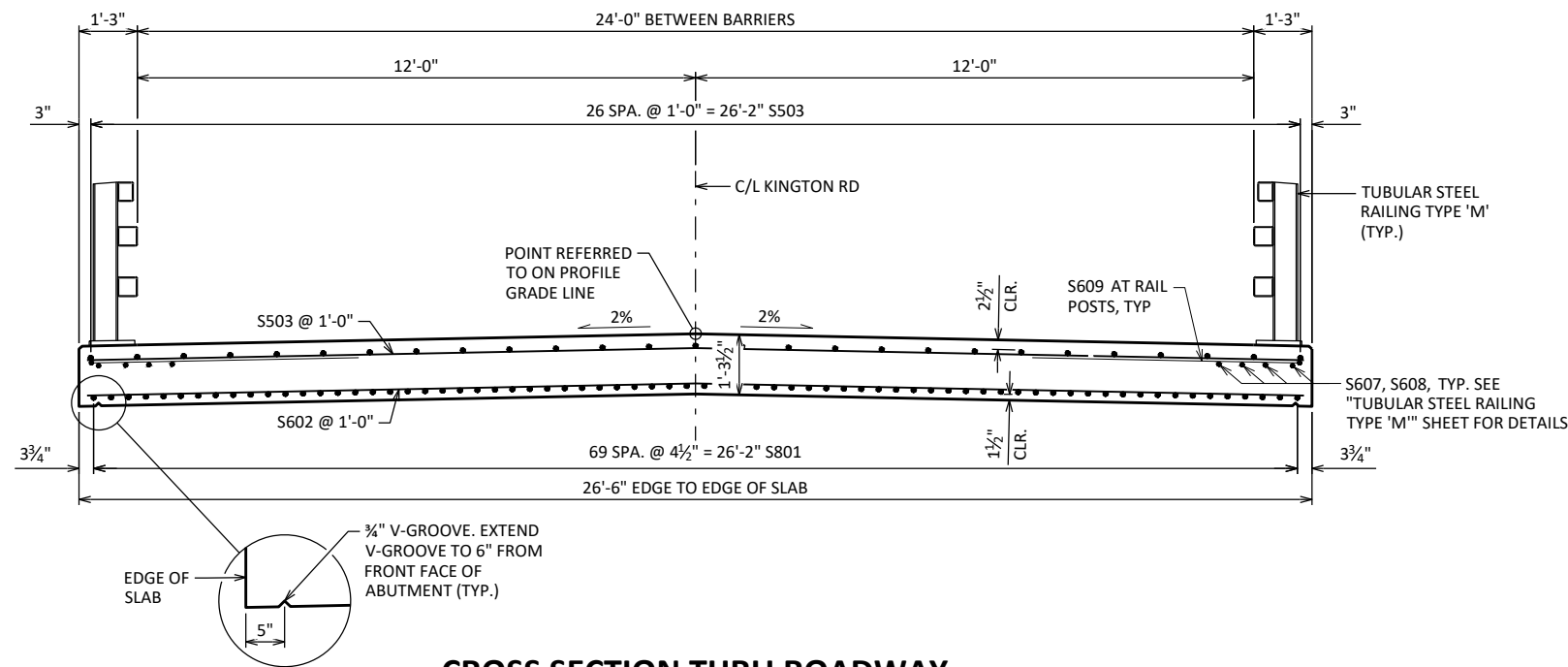
- (A01) OPTIONAL CONST. JOINT: KEYWAY FORMED BY A BEVELED 2X6. PROVIDE 3/4" "V" GROOVE ON F.F. OF WINGWALL IF JOINT IS USED.
- (A06) SUPPORT ABUTMENT ON HP 10 X 42 STEEL PILING, ESTIMATED 9 FEET LONG SEATED IN PRE-BORED HOLES CORE 3 FEET MINIMUM INTO SOUND BEDROCK. PILE DRIVING IS NOT REQUIRED. THE FACTORED AXIAL RESISTANCE OF PILES IN COMPRESSION USED FOR DESIGN IS 180 TONS MULTIPLIED BY A RESISTANCE FACTOR OF 0.5.
- (A15) PIPE UNDERDRAIN WRAPPED (6-INCH). SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. RODENT SHIELD REQUIRED.
- (A19) 18" RUBBERIZED MEMBRANE WATERPROOFING, ONLY IF OPTIONAL CONSTRUCTION JOINT IS USED. COST INCIDENTAL TO BID ITEM "CONCRETE MASONRY STRUCTURES".

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-10-0255			
DRAWN BY		DCV	PLANS CK'D TJR
EAST ABUTMENT DETAILS			SHEET 7

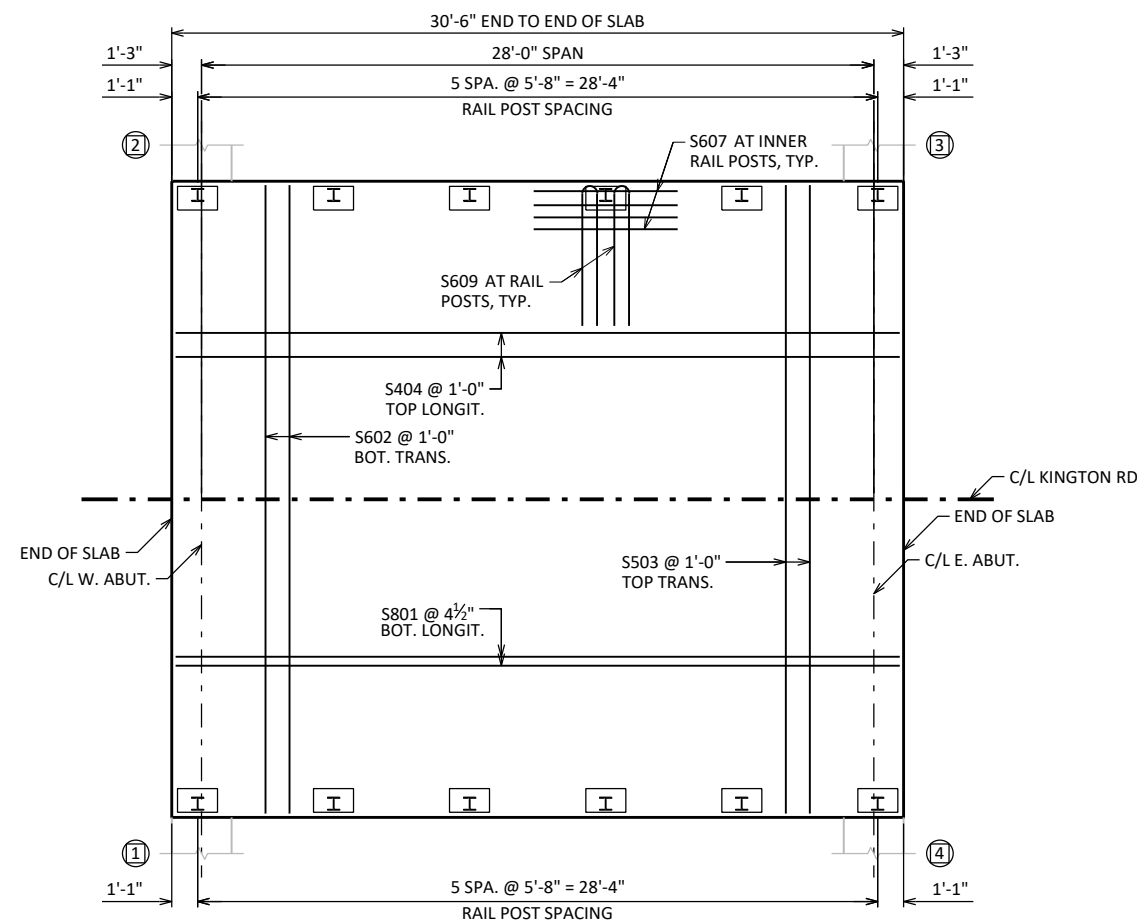
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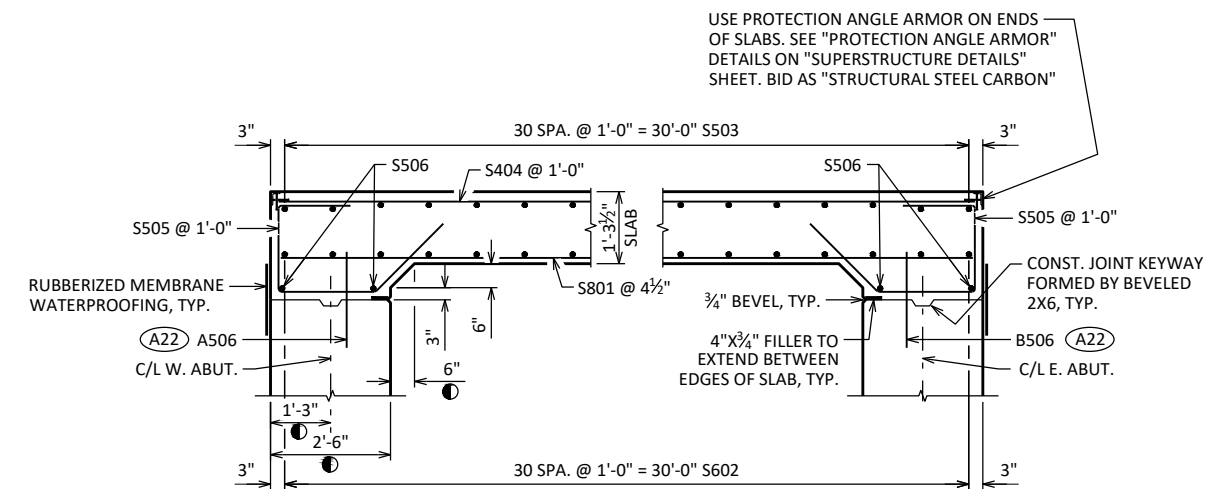
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CROSS SECTION THRU ROADWAY



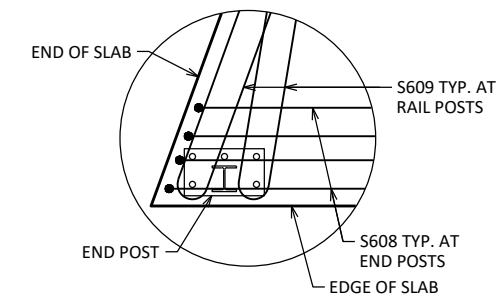
PLAN



LONGITUDINAL SECTION

DIMENSIONS ARE GIVEN PARALLEL TO ϵ ROADWAY UNLESS OTHERWISE NOTED.

- MEASURED NORMAL TO THE ϵ OF ABUTMENT. DIMENSIONS ARE TYPICAL FOR BOTH ABUTMENTS.
- A22 A506, B506 BARS SPACED @ 1'-0" CNTRS. MAY BE PLACED AFTER CONCRETE IS POURED BUT BEFORE INITIAL SET HAS TAKEN PLACE. (EMBED 1'-0" INTO CONC.)



END POST DETAILS

USE PROTECTION ANGLE ARMOR ON ENDS OF SLABS. SEE "PROTECTION ANGLE ARMOR" DETAILS ON "SUPERSTRUCTURE DETAILS" SHEET. BID AS "STRUCTURAL STEEL CARBON"

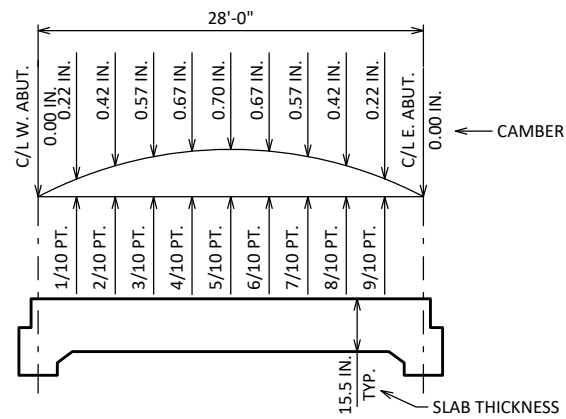


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NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-10-0255			
DRAWN BY		DCV	PLANS CK'D TJR
SUPERSTRUCTURE			SHEET 8

SCALE =



CAMBER AND SLAB THICKNESS DIAGRAM

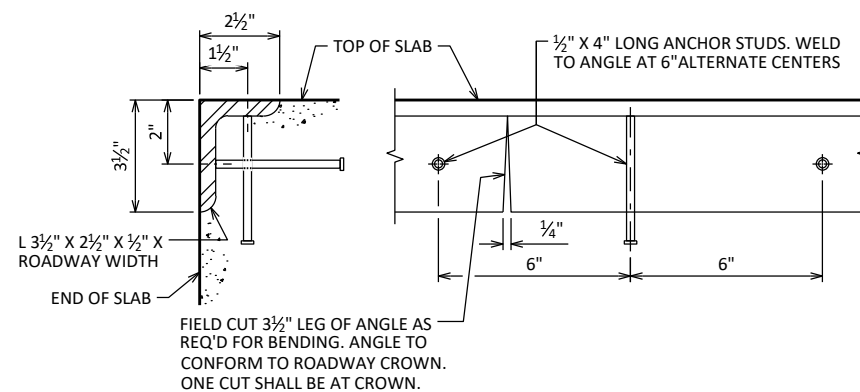
CAMBER SHOWN IS BASED ON 3 TIMES DEAD LOAD DEFLECTIONS. CAMBER SPANS AS SHOWN TO PROVIDE FOR DEAD LOAD DEFLECTION AND FUTURE CREEP. CAMBER DOES NOT INCLUDE ALLOWANCE FOR FORM SETTLEMENT. PARAPETS PLACED ON TOP OF THE SLAB SHALL BE POURED AFTER FALSEWORK HAS BEEN RELEASED, EXCEPT FOR STAGED CONSTRUCTION.

TO DETERMINE FALSEWORK ELEVATION AT EDGE OF SLAB, CROWN OR REFERENCE LINE FOLLOW THIS PROCEDURE:

- LESS TOP OF SLAB ELEVATION AT FINAL GRADE
- LESS SLAB THICKNESS
- PLUS CAMBER
- PLUS FORM SETTLEMENT/DEFLECTION DUE TO PLACEMENT OF SLAB CONCRETE (TO BE COMPUTED BY THE CONTRACTOR)
- EQUALS TOP OF SLAB FALSEWORK ELEVATION

TOP OF SLAB ELEVATIONS

LOCATION	C/L BRG. W. ABUT.	1/10 PT.	2/10 PT.	3/10 PT.	4/10 PT.	5/10 PT.	6/10 PT.	7/10 PT.	8/10 PT.	9/10 PT.	C/L BRG. E. ABUT.
N. EDGE OF DECK	1259.44	1259.44	1259.44	1259.44	1259.45	1259.45	1259.45	1259.46	1259.46	1259.47	1259.48
CROWN OR R/L	1259.70	1259.70	1259.70	1259.71	1259.71	1259.71	1259.72	1259.72	1259.73	1259.73	1259.74
S. EDGE OF DECK	1259.44	1259.44	1259.44	1259.44	1259.45	1259.45	1259.45	1259.46	1259.46	1259.47	1259.48

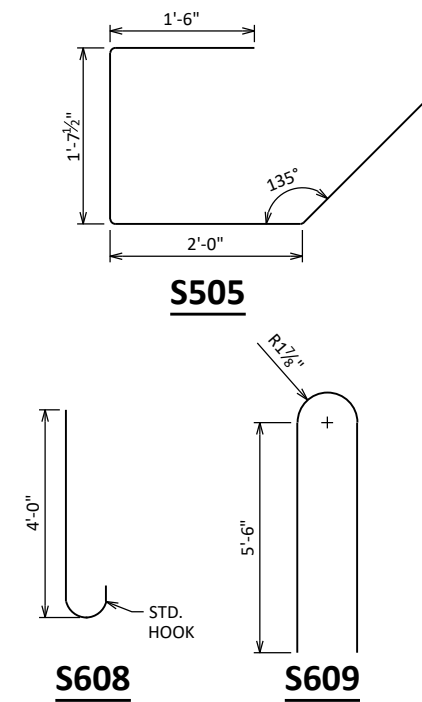


PROTECTION ANGLE ARMOR

SANDPLAST PROTECTION ANGLE AFTER FABRICATION IN ACCORDANCE WITH SSPC SP. #6 "COMMERCIAL BLAST CLEANING" AFTER BLAST CLEANING, THE PROTECTION ANGLE SHALL BE HOT DIPPED GALVANIZED.

ANGLE AND STUDS TO BE PAID FOR AT THE UNIT PRICE BID FOR "STRUCTURAL STEEL CARBON". (NO PAINT REQ'D.)

NO SPLICE SHALL BE PERMITTED IN ANGLES.



BILL OF BARS

NOTE: THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE.

BAR MARK	COAT	NO. REQ'D.	LENGTH	BENT	BAR SERIES	LOCATION
S801	X	70	30'-2"			SLAB BOTTOM LONGITUDINAL
S602	X	31	26'-2"			SLAB BOTTOM TRANSVERSE
S503	X	31	26'-2"			SLAB TOP TRANSVERSE
S404	X	27	30'-2"			SLAB TOP LONGITUDINAL
S505	X	54	6'-11"	X		ABUTMENT DIAPHRAGM STIRRUPS
S506	X	4	26'-2"			ABUTMENT DIAPHRAGM LONGITUDINAL
S607	X	32	6'-0"			SLAB TOP LONGIT. UNDER RAIL POSTS
S608	X	16	4'-8"	X		SLAB TOP LONGIT. UNDER RAIL END POSTS
S609	X	24	12'-0"	X		SLAB TOP HOOKS UNDER RAIL POSTS

SURVEY TOP OF SLAB ELEVATIONS

LOCATION	ABUTMENT	5/10 PT.	ABUTMENT
N. GUTTER			
CROWN OR R/L			
S. GUTTER			

PRIOR TO RELEASING SLAB FALSEWORK, TAKE TOP OF SLAB ELEVATIONS AT THE C/L OF ABUTMENTS, THE C/L OF PIERS AND AT 5/10 PTS. TO VERIFY CAMBER. TAKE ELEVATIONS ALONG GUTTER LINES AND CROWN OR R/L. RECORD THE ELEVATIONS IN THE ABOVE TABLE FOR THE "AS BUILT" PLANS.

NOTES

FILL IN THE TABLE OF "SURVEY TOP OF SLAB ELEVATIONS" FOR EACH SPAN ON AS BUILT PLANS.

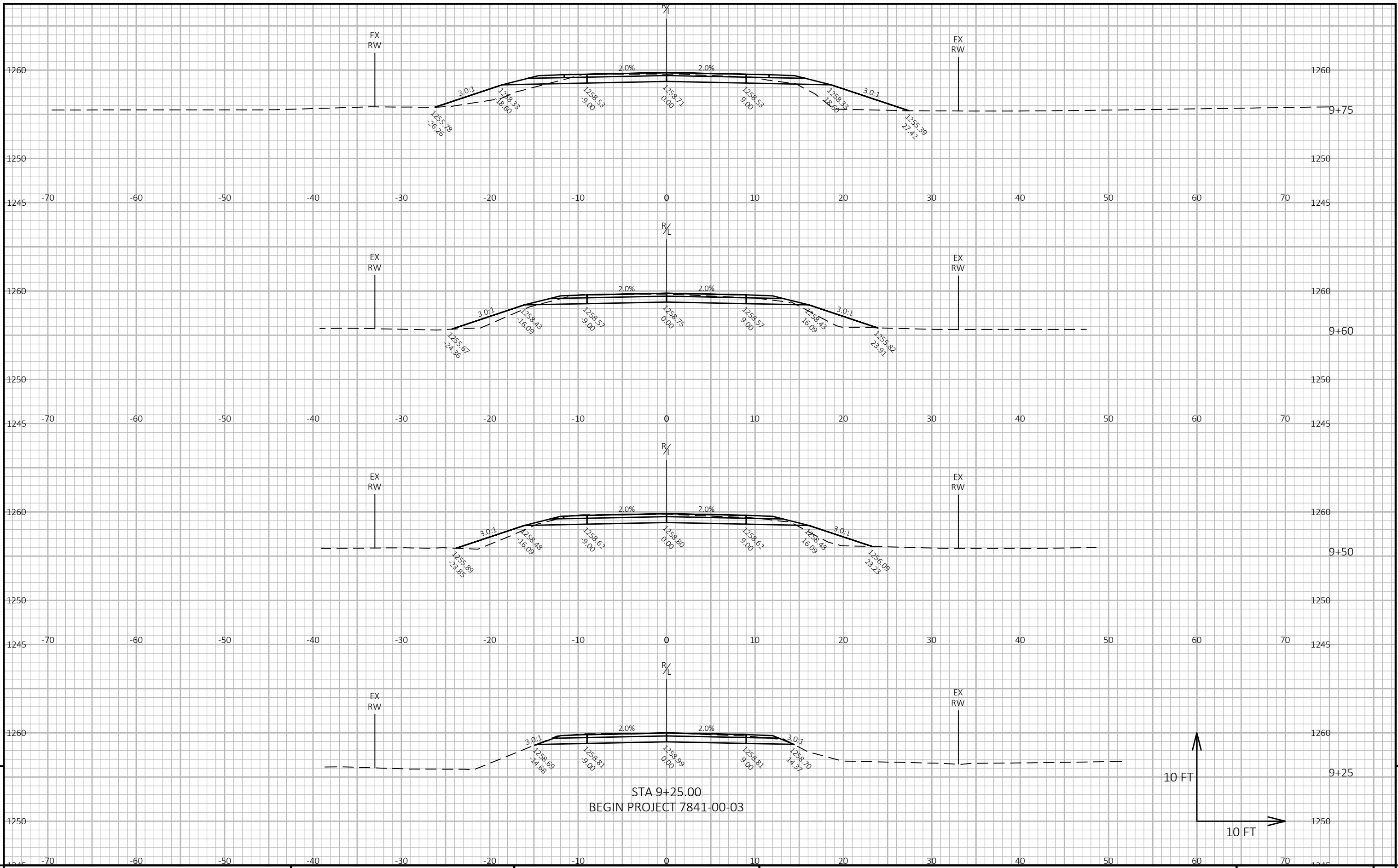
TOP TRANSVERSE BARS IN SLAB SHALL BE SUPPORTED BY INDIVIDUAL BAR CHAIRS AT APPROXIMATELY 3'-0" CENTERS EACH WAY. BOTTOM LONGITUDINAL BARS SHALL BE SUPPORTED BY CONTINUOUS BAR CHAIRS AT APPROXIMATELY 4'-0" CENTERS.

ALL SLAB THICKNESS DIMENSIONS ARE MINIMUM. ANY TOLERANCES NECESSARY TO CORRECT CONSTRUCTION DISCREPANCIES ARE TO BE PLUS (+).

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-10-0255			
DRAWN BY		DCV	PLANS CK'D TJR
SUPERSTRUCTURE DETAILS			SHEET 9

KINGTON RD

STATION	AREA (SF)			INCREMENTAL VOL (CY) (UNADJUSTED)			CUMULATIVE VOL (CY)				
	CUT	SALVAGED/UNUSABLE PAVEMENT MATERIAL	FILL	CUT	SALVAGED/UNUSABLE PAVEMENT MATERIAL	FILL	CUT	EXPANDED FILL		MASS ORDINATE	
								1.00	1.25		
								NOTE 1	NOTE 4		
9+25	25	0	0	0	0	0	0	0	0		
9+50	24	0	11	23	0	5	23	7	16		
9+60	22	0	13	8	0	4	31	12	19		
9+75	19	0	26	12	0	11	43	25	17		
9+78	20	0	29	2	0	3	45	29	15		
10+22	27	0	35	0	0	0	45	29	15		
10+27	26	0	24	5	0	5	50	36	13		
10+40	24	0	13	12	0	9	62	47	14		
10+50	24	0	12	9	0	5	71	53	17		
10+75	20	0	0	20	0	6	91	60	31		
				91	0	48					

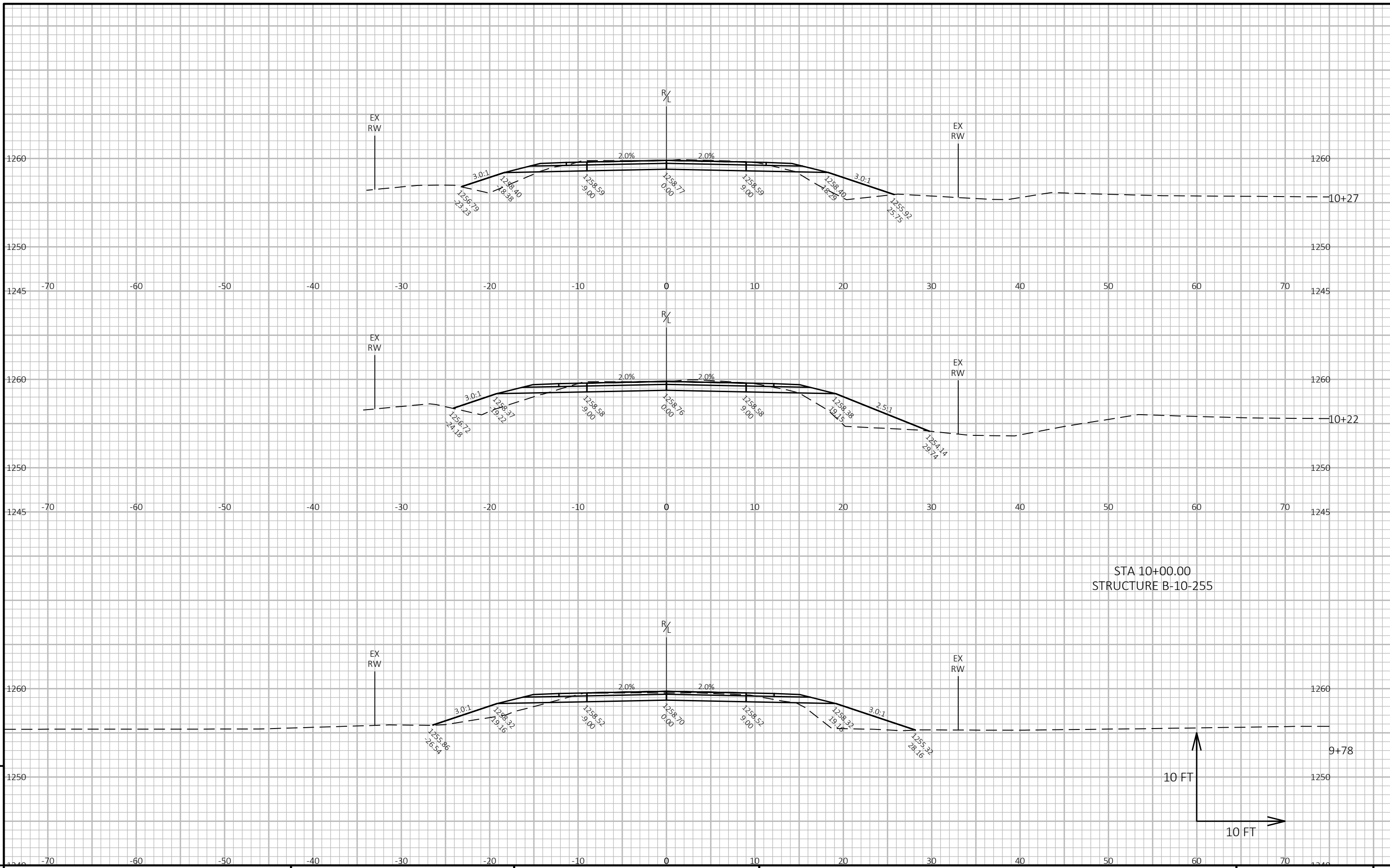


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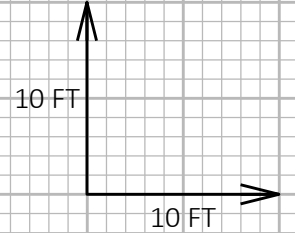
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PROJECT NO: 7841-00-03 HWY: LOCAL STREET COUNTY: CLARK CROSS SECTIONS: KINGTON RD SHEET E

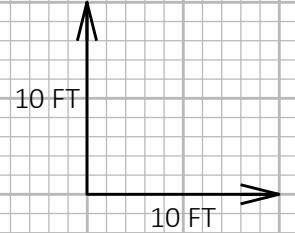
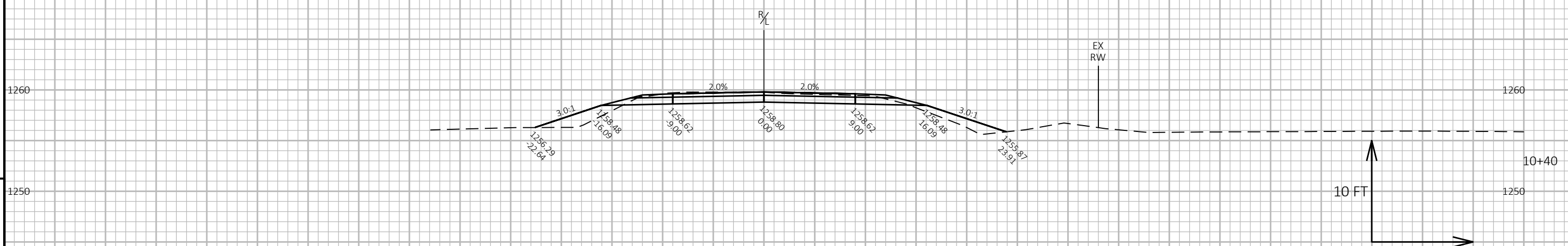
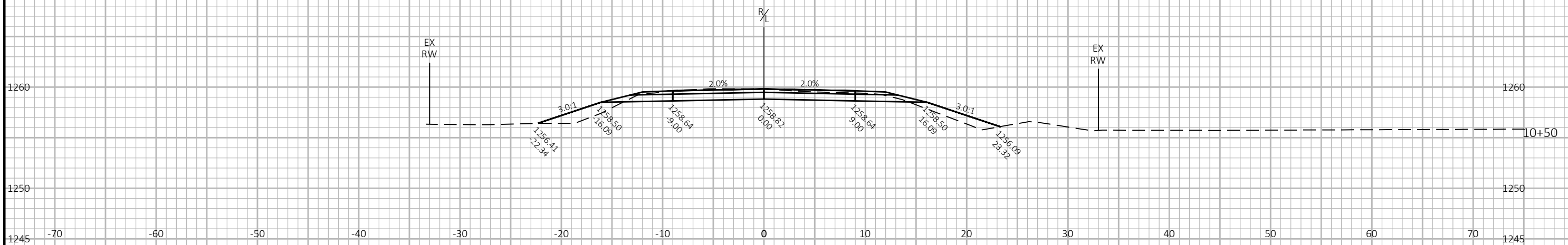
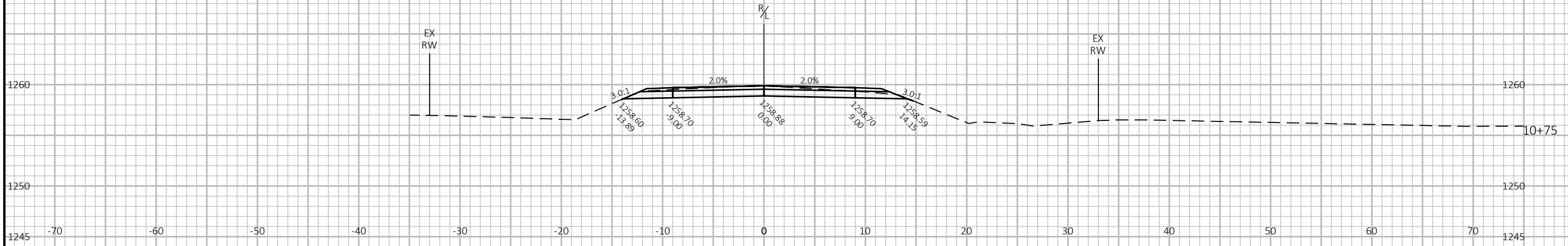
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STA 10+00.00
STRUCTURE B-10-255



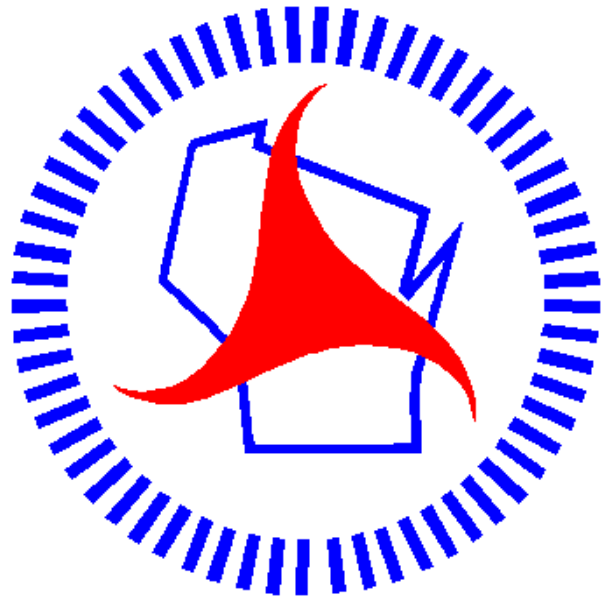
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END PROJECT 7841-00-03



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PROJECT NO: 7841-00-03 HWY: LOCAL STREET COUNTY: CLARK CROSS SECTIONS: KINGTON RD SHEET E

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Wisconsin Department of Transportation

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