

wke
PROJECT ID:
WITH: N/A

November 2024

2720-07-71

COUNTY:
WAUKESHA

ORDER OF SHEETS

Section No.	Title
1	
2	Typical Sections and Details
3	Estimate of Quantities
3	Miscellaneous Quantities
4	Right of Way Plat
5	Plan and Profile
6	Standard Detail Drawings
7	Sign Plates
8	Structure Plans
9	Computer Earthwork Data
9	Cross Sections

TOTAL SHEETS = 474



DESIGN DESIGNATION

A.A.D.T. (2025)	=	7,900
A.A.D.T. (2045)	=	8,700
D.H.V. (2045)	=	870
D.D.	=	59/41
T.	=	7.0 %
DESIGN SPEED	=	35 MPH
ESALS	=	744,600

CONVENTIONAL SYMBOLS

PLAN	PROFILE
CORPORATE LIMITS	GRADE LINE
PROPERTY LINE	ORIGINAL GROUND
LOT LINE	MARSH OR ROCK PROFILE (To be noted as such)
LIMITED HIGHWAY EASEMENT	SPECIAL DITCH
EXISTING RIGHT OF WAY	GRADE ELEVATION
PROPOSED OR NEW R/W LINE	CULVERT (Profile View)
SLOPE INTERCEPT	UTILITIES
REFERENCE LINE	ELECTRIC
EXISTING CULVERT	FIBER OPTIC
PROPOSED CULVERT (Box or Pipe)	GAS
COMBUSTIBLE FLUIDS	SANITARY SEWER
MARSH AREA	STORM SEWER
WOODED OR SHRUB AREA	TELEPHONE
	WATER
	UTILITY PEDESTAL
	POWER POLE
	TELEPHONE POLE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

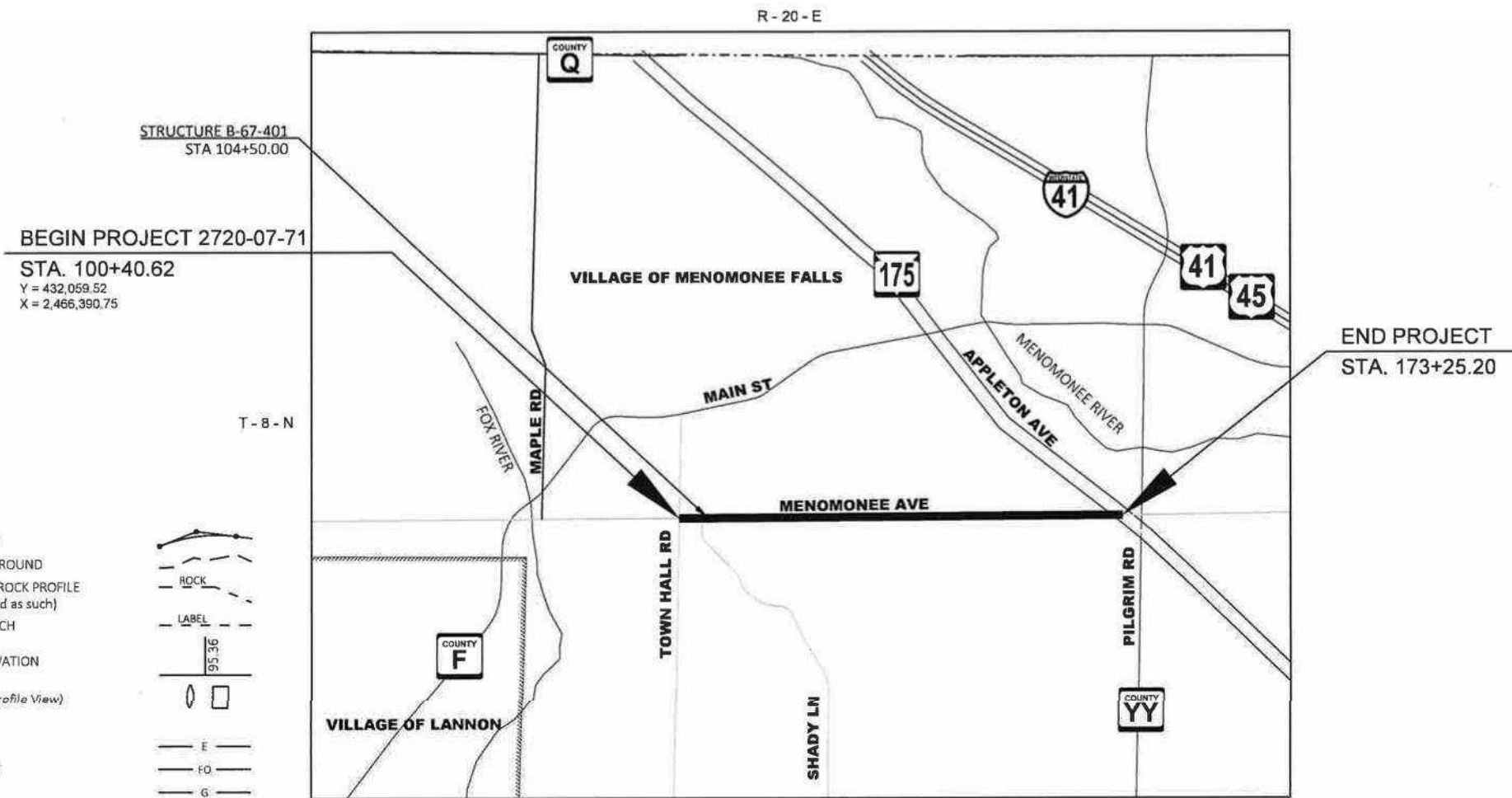
V MENOMONEE FALLS MENOMONEE AVE

TOWN HALL ROAD TO STH 175

LOCAL STREET

WAUKESHA COUNTY

STATE PROJECT NUMBER
2720-07-71



LAYOUT
SCALE 0 0.5 MI
TOTAL NET LENGTH OF CENTERLINE = 1.380 MI

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN STATE PLANE COORDINATE SYSTEM (WSPCS) GRID, SOUTH ZONE NAD83 (2011), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES.

ELEVATIONS ARE REFERENCED TO NAVD 88 (2012). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 12A.

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
2720-07-71	wisc 2025033	1

ACCEPTED FOR
VILLAGE of MENOMONEE FALLS

Thomas M. Hoffman, P.E.
(PRINTED NAME) *[Signature]*
(SIGNATURE)

July 30, 2024
(DATE) Director of Eng. & Development
(TITLE OF OFFICIAL)

ORIGINAL PLANS PREPARED BY

raSmith
CREATIVITY BEYOND ENGINEERING
rasmith.com

WISCONSIN
CALEB L. MANSKE
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BROOKFIELD
WI
PROFESSIONAL ENGINEER

07/25/2024
(DATE) *[Signature]*
(SIGNATURE)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY

Surveyor	raSmith
Designer	raSmith
Project Manager	JOSEPH JELACIC
Regional Examiner	
Regional Supervisor	BRIAN BOOTHBY

APPROVED FOR THE DEPARTMENT

Joseph Jelacic
(Signature)

DATE: 7/31/2024

E

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GENERAL NOTES

- NO TREES OR SHRUBS ARE TO BE REMOVED WITHOUT APPROVAL OF THE ENGINEER.
- BACKFILL ALL OPENINGS OF HOLES BELOW SUBGRADE RESULTING FROM REMOVALS OR ABANDONMENTS IN ACCORDANCE WITH SECTION 204 OF THE STANDARD SPECS. ALL BACKFILL MATERIAL IS INCIDENTAL TO THE REMOVAL ITEM.
- EROSION CONTROL DEVICES ARE AT SUGGESTED LOCATIONS. ACTUAL LOCATION ARE TO BE DETERMINED BY THE CONTRACTORS ECIP AND BY THE ENGINEER. MAINTAIN EROSION CONTROL DEVICES UNTIL PERMANENT VEGETATION IS ESTABLISHED OR UNTIL THE ENGINEER DETERMINES THAT THE DEVICE IS NO LONGER REQUIRED.
- PLACE EROSION CONTROL DEVICES IN SEQUENCE WITH CONSTRUCTION OPERATIONS OR AS DETERMINED BY THE ENGINEER.
- RE-TOPSOIL GRADED AREAS, AS DESIGNATED BY THE ENGINEER, IMMEDIATELY AFTER GRADING IS COMPLETED WITHIN THOSE AREAS. SOD AND FERTILIZE TOPSOILED AREAS, AS DESIGNATED BY THE ENGINEER, WITHIN FOUR (4) CALENDAR DAYS AFTER PLACEMENT OF TOPSOIL. IF GRADED AREAS ARE TO BE LEFT EXPOSED FOR MORE THAN FOURTEEN (14) CALENDAR DAYS, SEED THOSE AREAS WITH TEMPORARY SEED AND MULCH.
- TEMPORARILY STABILIZE ANY DISTURBED AREAS WITHIN 50 FEET OF ANY WETLAND, WATERWAY OR CULVERT GREATER THAN 24-INCHES WITH TEMPORARY SEED AND MULCH WITHIN 48-HOURS.
- STOCKPILE EXCESS MATERIAL OR SPOILS ON UPLAND AREAS AWAY FROM WETLANDS, FLOODPLAINS, AND WATERWAYS. PROTECT STOCKPILED SOIL AGAINST EROSION. IF STOCKPILED MATERIAL IS TO BE LEFT FOR MORE THAN FOURTEEN (14) CALENDAR DAYS, RESTORE THE STOCKPILE WITH TEMPORARY SEED AND MULCH.
- RESHAPE, RESTORE, AND FINISH ALL PREVIOUSLY GRASSED AREAS DISTURBED OUTSIDE THE NORMAL CONSTRUCTION LIMITS AT NO EXPENSE TO THE DEPARTMENT.
- PLACE TOPSOIL ONE INCH BELOW THE TOP OF ADJACENT CURBS OR SIDEWALKS IN SOD AREAS.
- THE LOCATIONS OF EXISTING UTILITY INSTALLATIONS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS IN THE AREA THAT ARE NOT SHOWN. THE CONTRACTOR SHALL NOTIFY DIGGERS HOTLINE AND AFFECTED UTILITIES PRIOR TO THE START OF WORK. ANY MUNICIPAL UTILITY WHICH IS NOT A MEMBER OF DIGGERS HOTLINE MUST BE CONTACTED SEPARATELY.
- STATIONING, DISTANCES, AND OFFSETS FOR SIGNS SHOWN ON THE PLANS ARE APPROXIMATE AND THE LOCATIONS OF SIGNS ARE TO BE DETERMINED IN THE FIELD BY THE ENGINEER. ALL SIGN LOCATIONS SHALL BE REVIEWED BY THE ENGINEER PRIOR TO INSTALLATION.
- VERIFY EXISTING PAVEMENT ELEVATIONS AT ALL TIE-INS TO EXISTING PAVEMENT PRIOR TO CONSTRUCTION. NOTIFY ENGINEER IF A DISCREPANCY IS FOUND BETWEEN PROPOSED PLAN ELEVATIONS AND EXISTING PAVEMENT ELEVATIONS.
- CONTRACTOR MUST CONTACT THE ENGINEER AND SEWRPC AT LEAST TWO WEEKS NOTICE PRIOR TO WORK NEAR ANY PUBLIC SURVEY MONUMENT.
- CURB AND GUTTER STATIONS, OFFSETS, ELEVATIONS, AND RADII ARE MEASURED AT THE FLANGE LINE UNLESS NOTED OTHERWISE.
- A SAWED JOINT WILL BE REQUIRED WHERE NEW PAVEMENT IS TO MEET AN EXISTING PAVED SURFACE. SQUEEGEE AND REMOVE THE SAW CUT SLURRY FROM PAVEMENT BEFORE MOVING ON TO THE NEXT SAW CUT LOCATION.

ORDER OF SECTION 2 SHEETS

- GENERAL NOTES
- PROJECT OVERVIEW
- TYPICAL SECTIONS
- CONSTRUCTION DETAILS
- PLAN DETAILS
- CURB RAMP DETAILS
- EROSION CONTROL
- STORM SEWER
- SIGNING AND PAVEMENT MARKING
- TRAFFIC CONTROL
- DETOUR PLAN
- ALIGNMENT

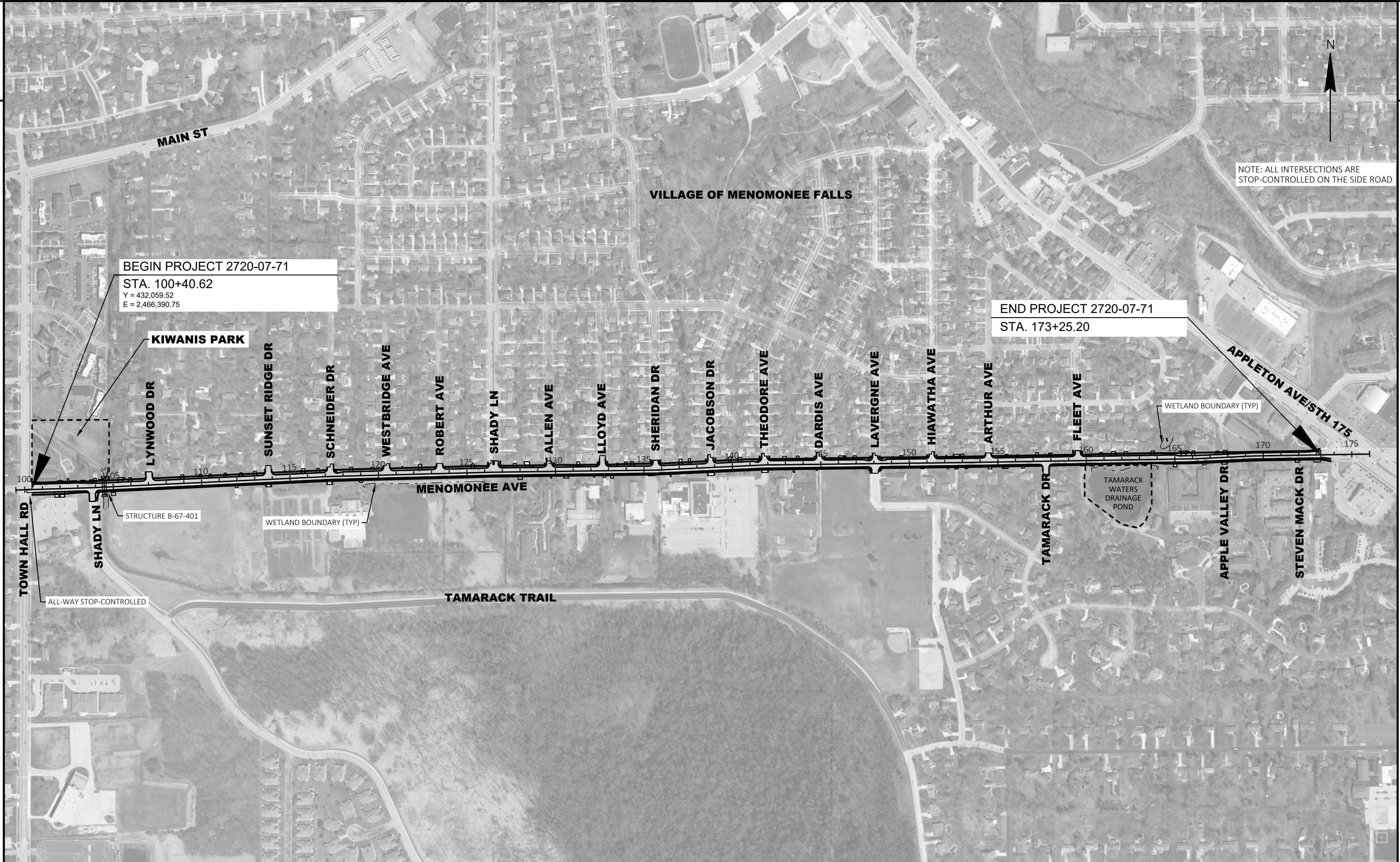
RUNOFF COEFFICIENT TABLE

	HYDROLOGIC SOIL GROUP											
	A			B			C			D		
	SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)		
LAND USE:	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER
ROW CROPS:	.08	.16	.22	.12	.20	.27	.15	.24	.33	.19	.28	.38
MEDIAN STRIPTURF:	.22	.30	.38	.26	.34	.44	.30	.37	.50	.34	.41	.56
SIDE SLOPETURF:	.19	.20	.24	.19	.22	.26	.20	.23	.30	.20	.25	.30
PAVEMENT:												
ASPHALT:												
CONCRETE:												
BRICK:												
DRIVES, WALKS:												
ROOFS:												
GRAVEL ROADS, SHOULDERS:												

TOTAL PROJECT AREA = 14.914 ACRES
TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = 13.193 ACRES

DIGGERS HOTLINE
Dial 811 or (800)242-8511
www.DiggersHotline.com

LAYER	THICKNESS	TYPE
UPPER	2 1/4-INCHES	4 MT 58-28 S
LOWER	2 3/4-INCHES	3 MT 58-28 S



NOTE: ALL INTERSECTIONS ARE STOP-CONTROLLED ON THE SIDE ROAD

BEGIN PROJECT 2720-07-71
 STA. 100+40.62
 Y = 432,059.52
 E = 2,466,390.75

END PROJECT 2720-07-71
 STA. 173+25.20

KIWANIS PARK

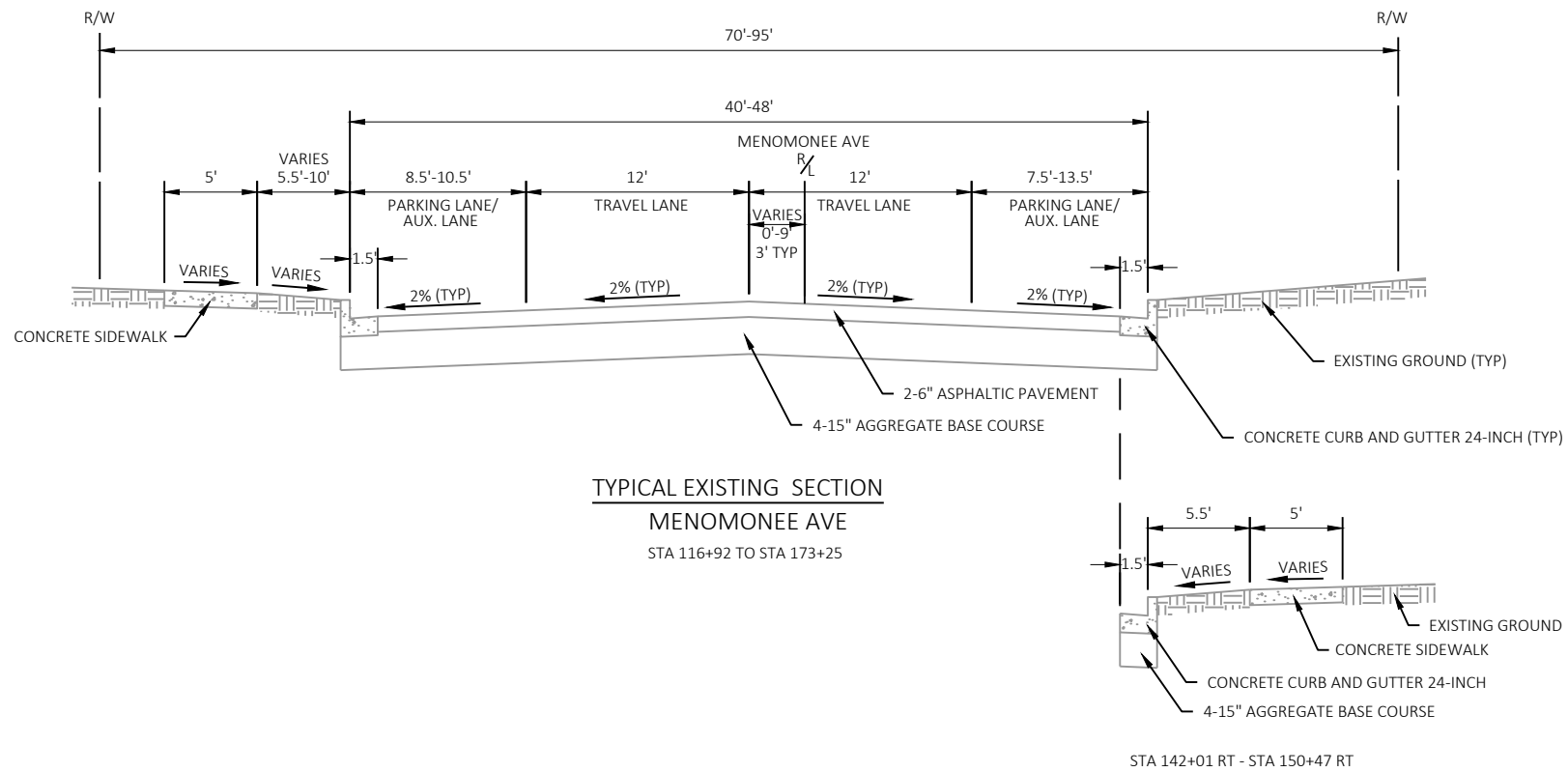
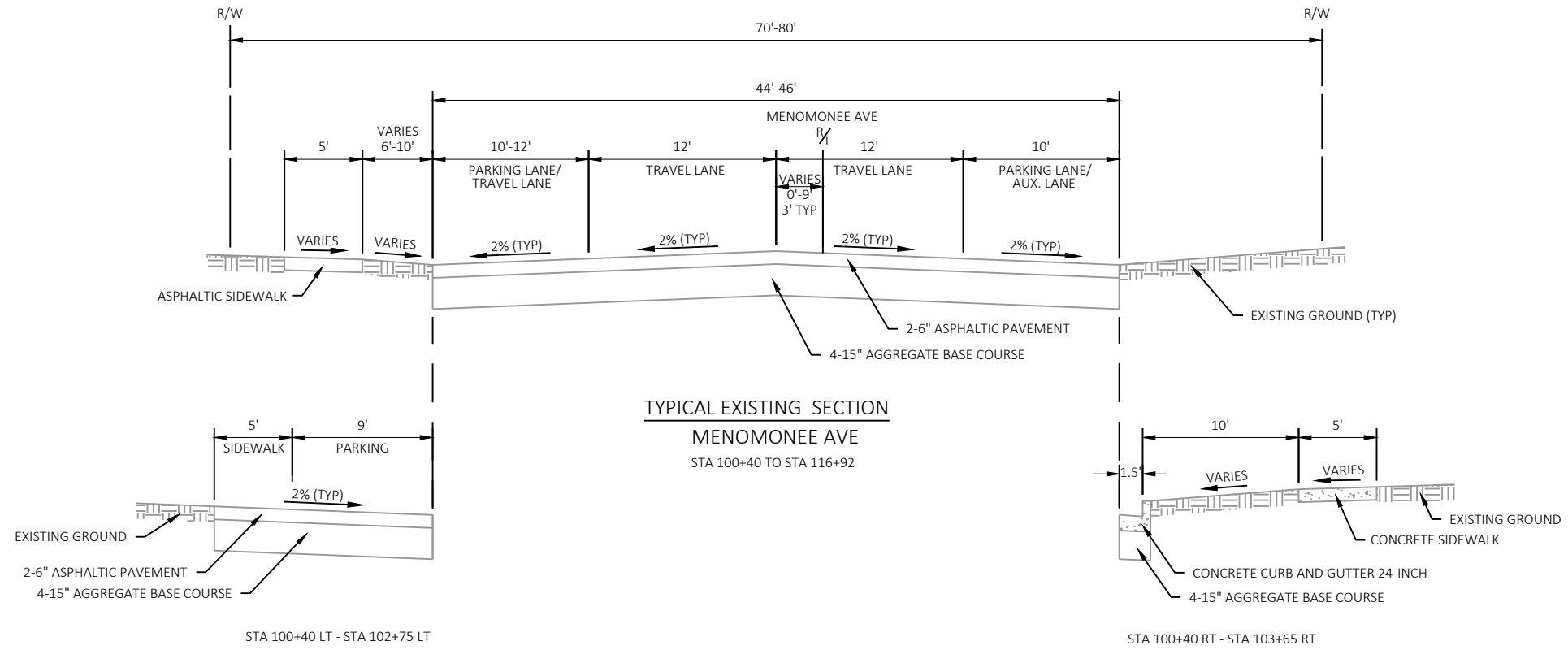
WETLAND BOUNDARY (TYP)

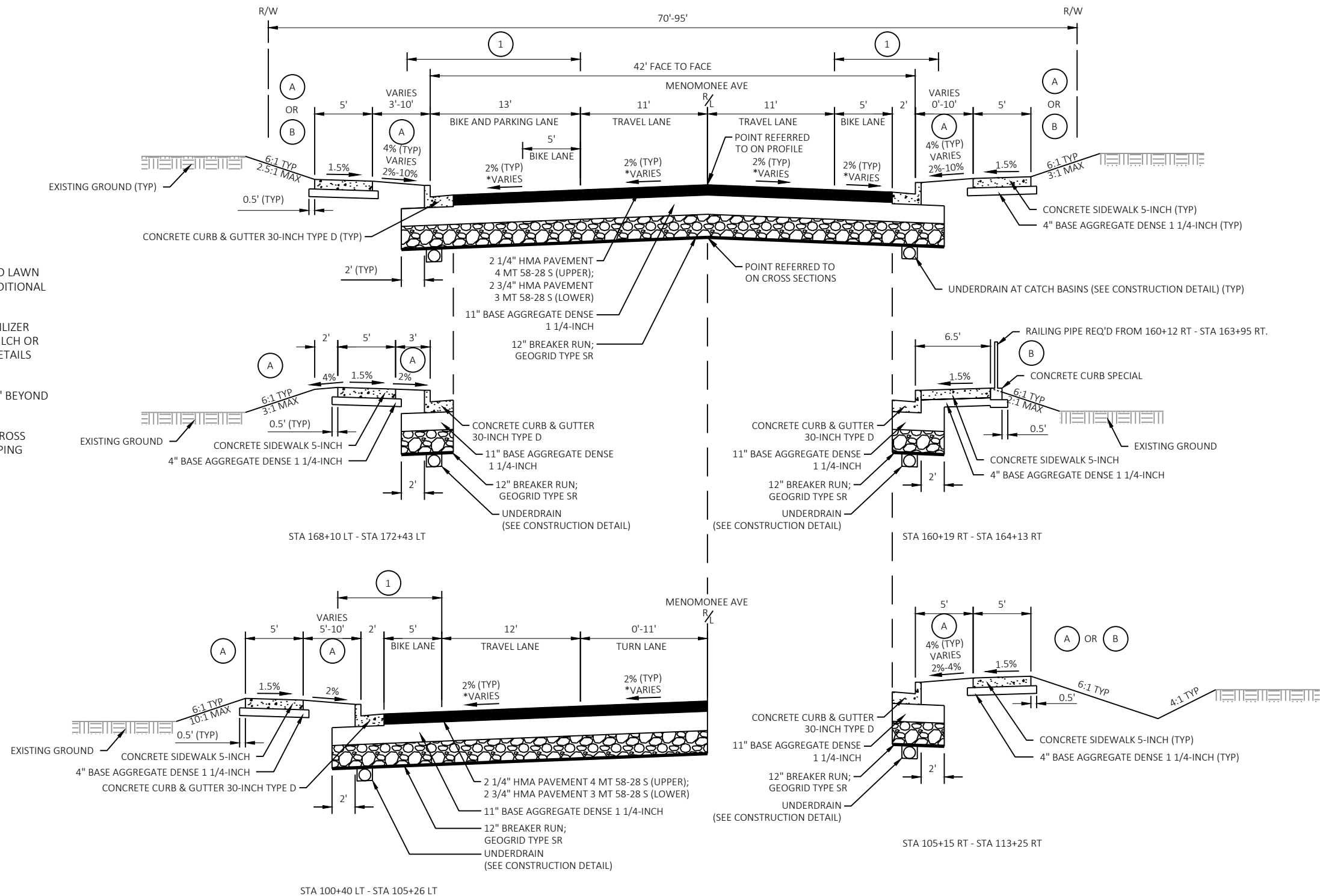
STRUCTURE B-67-401

WETLAND BOUNDARY (TYP)

ALL-WAY STOP-CONTROLLED

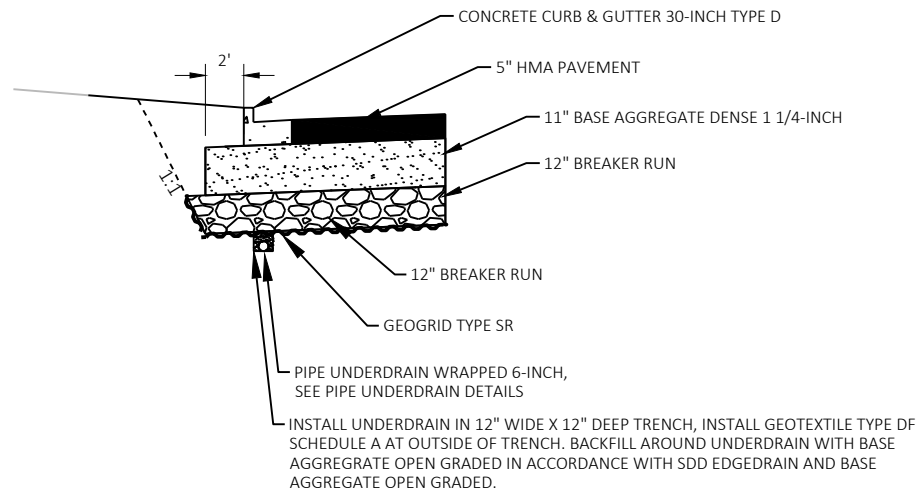
PROJECT NO: 2720-07-71	HWY: MENOMONEE AVE	COUNTY: WAUKESHA	PROJECT OVERVIEW	SHEET	E
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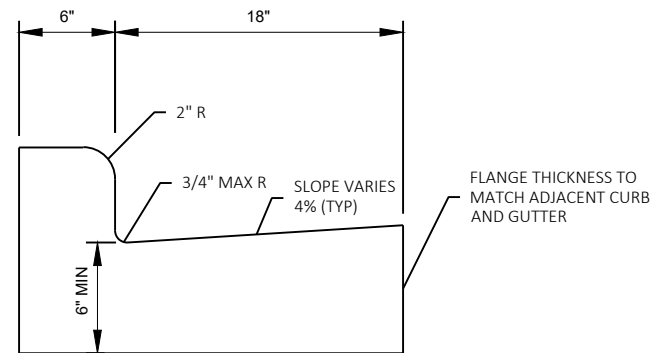


TYPICAL FINISHED SECTION
 MENOMONEE AVE
 STA 100+40 TO STA 173+25
 DESIGN SPEED = 35 MPH

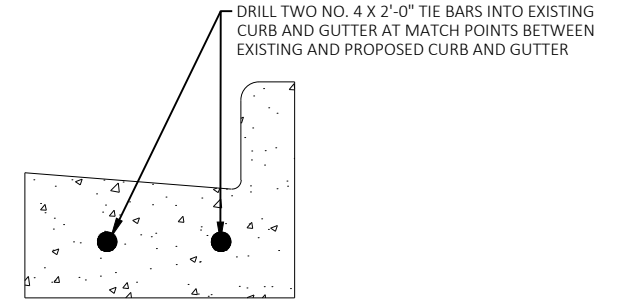
- (A) TOPSOIL; FERTILIZER TYPE B; SOD LAWN (TYP) SEE PLAN DETAILS FOR ADDITIONAL INFORMATION
 - (B) SEEDING MIXTURE NO. 40; FERTILIZER TYPE B; SALVAGED TOPSOIL; MULCH OR EROSION MAT (TYP) SEE PLAN DETAILS FOR ADDITIONAL INFORMATION
 - (1) LATERAL CLEARANCE EXTENDS 2' BEYOND THE FACE OF CURB
- *REFER TO PLAN DETAILS AND CROSS SECTIONS FOR PAVEMENT WARPING LOCATIONS



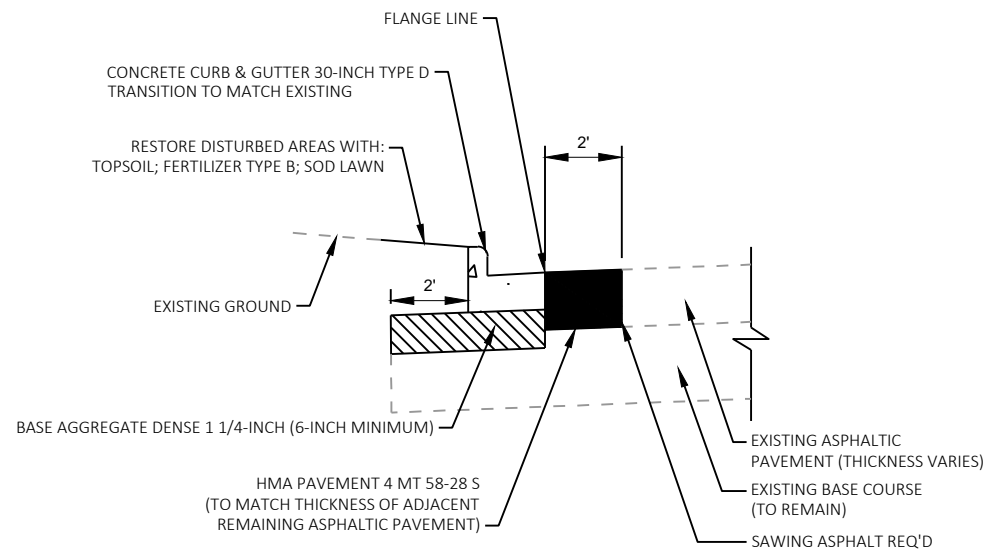
SUBGRADE IMPROVEMENT WITH GEOGRID



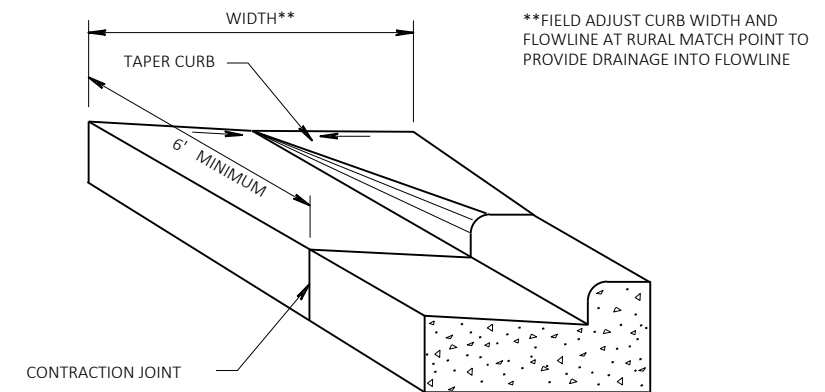
CONCRETE CURB & GUTTER 24-INCH TYPE D
SEE PLAN DETAILS FOR LOCATIONS



DRILLED TIE BARS AND CURB & GUTTER MATCH POINTS
CONSTRUCT AT ALL LOCATIONS WHERE PROPOSED CURB & GUTTER MATCHES EXISTING CURB & GUTTER



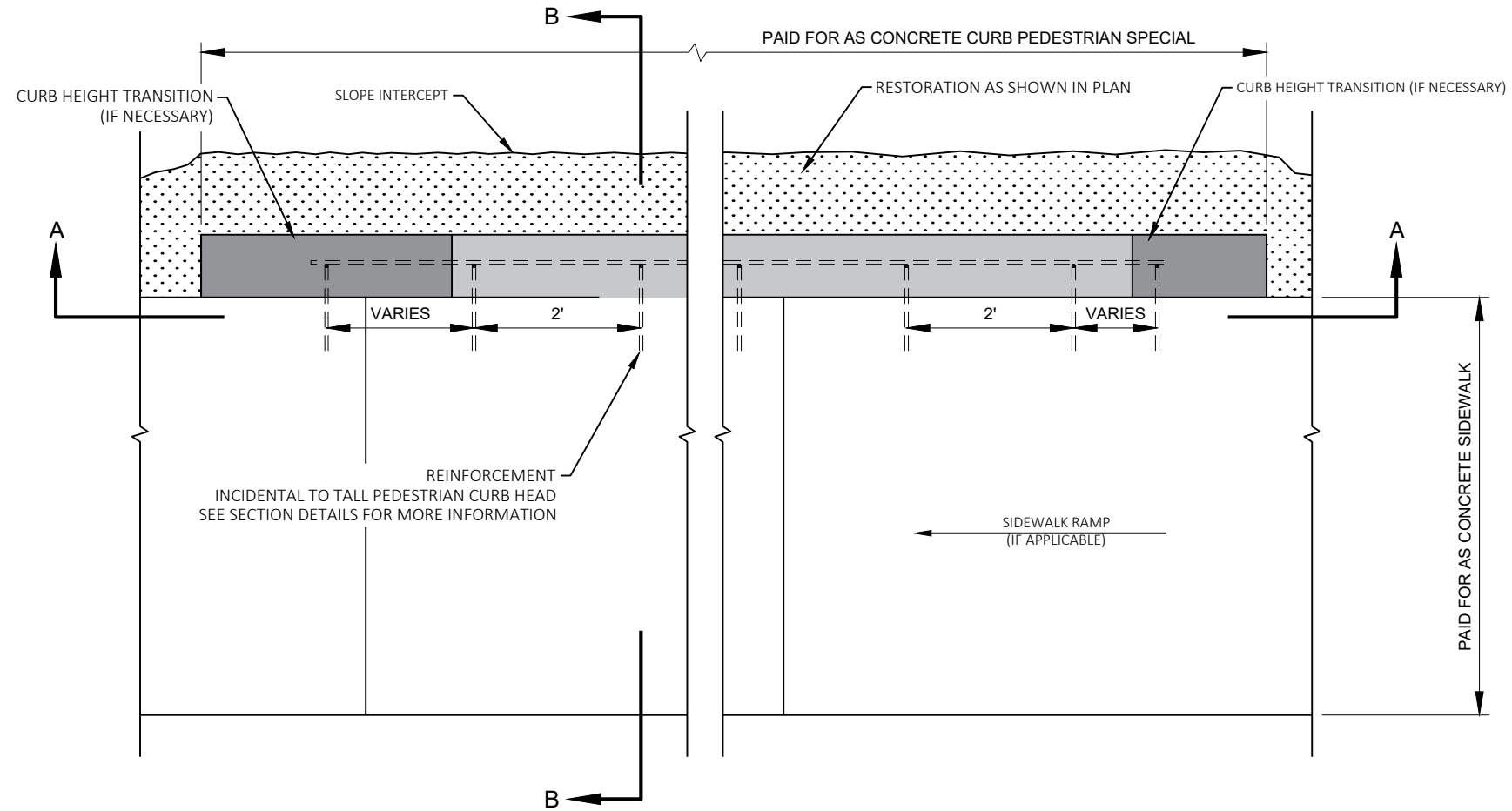
CONCRETE CURB & GUTTER SPOT REPLACEMENT
EAST OF 173+25



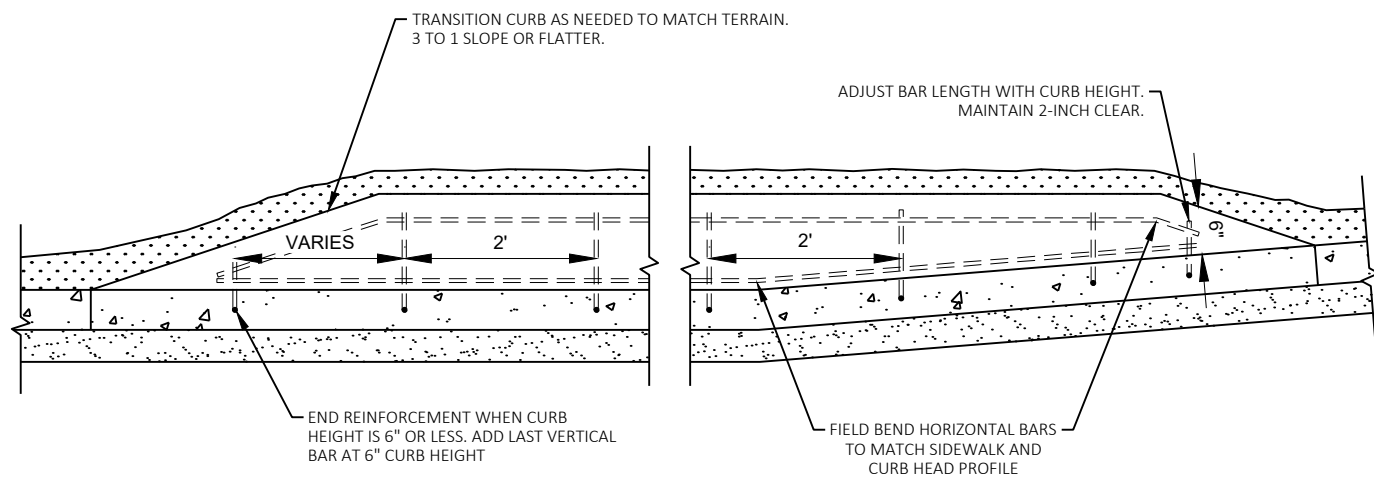
DETAIL OF CURB & GUTTER TERMINI
LYNWOOD DRIVE
SUNSET RIDGE DRIVE

NOTES

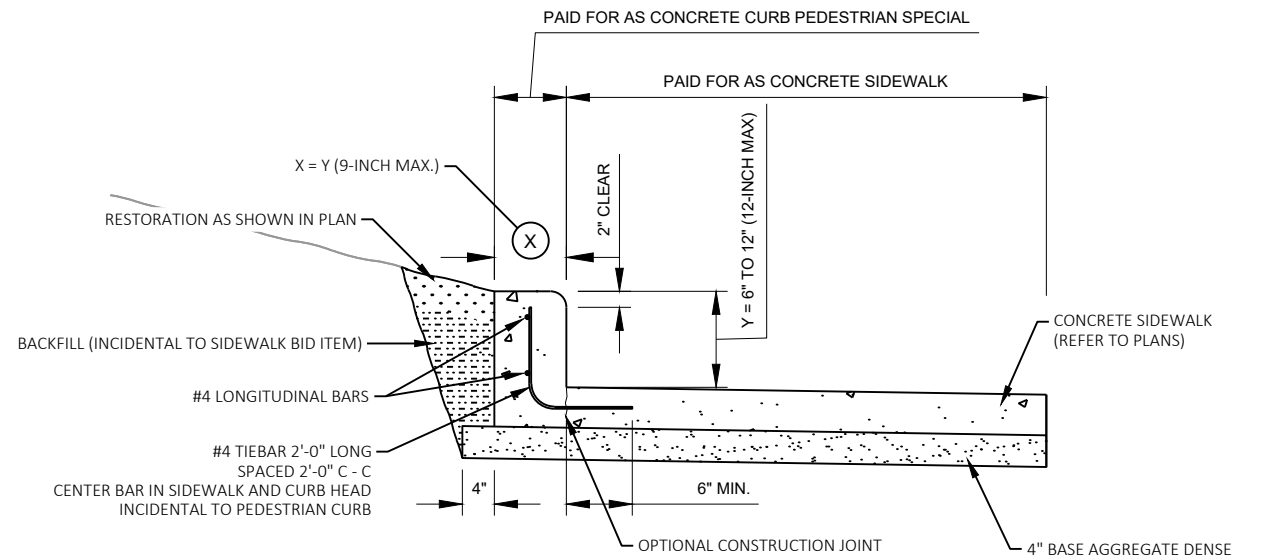
- 1. CONSTRUCTION FOR TALL PEDESTRIAN CURB SHALL CONFORM TO STANDARD SPEC 601.
- 2. PEDESTRIAN CURB IS INTENDED TO BE FORMED AND POURED INTEGRAL WITH ADJACENT SIDEWALK. NO ADDITIONAL PAYMENT WILL BE MADE FOR DRILLING TIE BARS IF CONTRACTOR CHOOSES TO POUR CURB AND SIDEWALK SEPARATELY.



PLAN VIEW



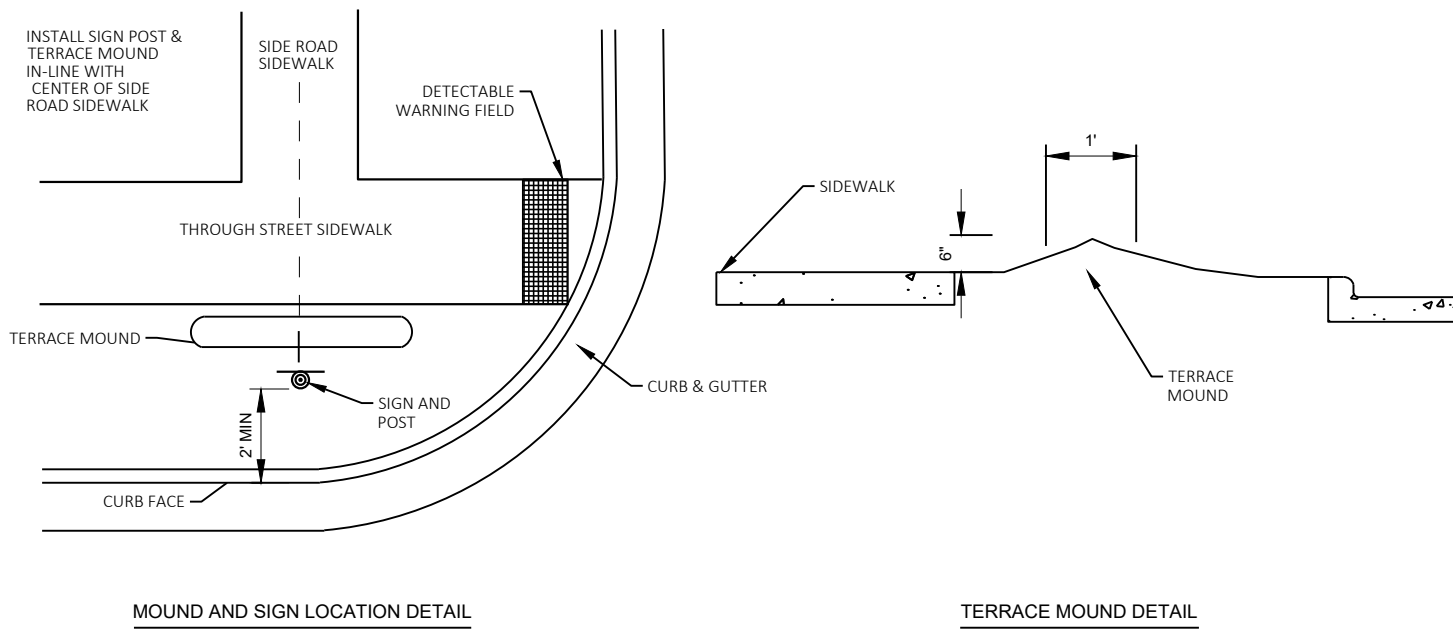
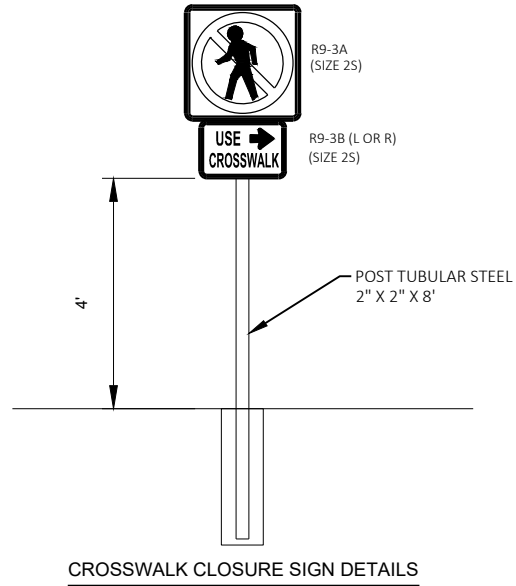
SECTION A - A



SECTION B - B

CONCRETE CURB PEDESTRIAN SPECIAL

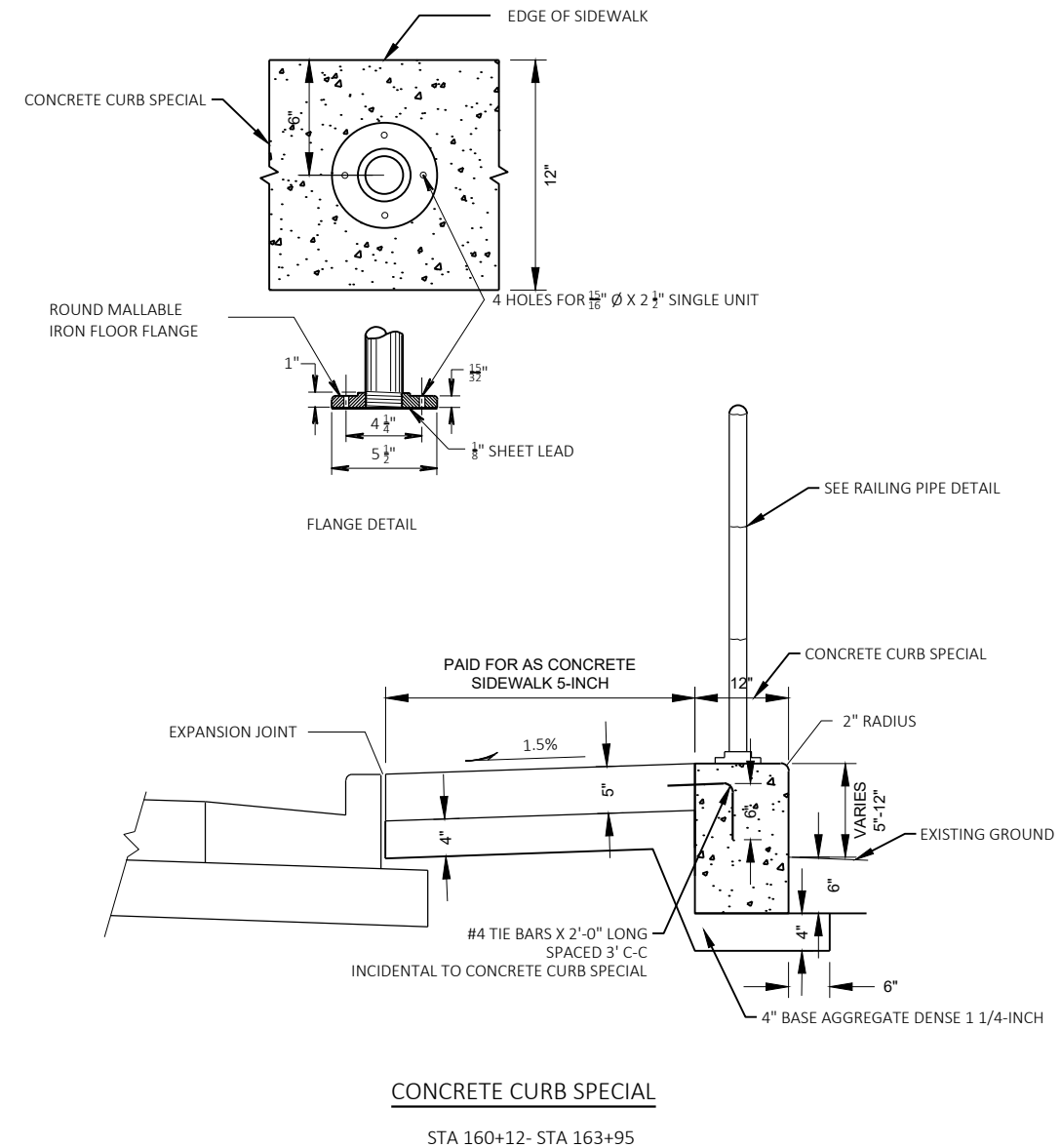
FOR PEDESTRIAN CURB GREATER THAN 6-INCHES IN HEIGHT. REFER TO CURB RAMP DETAILS FOR LOCATION AND CURB HEIGHT.

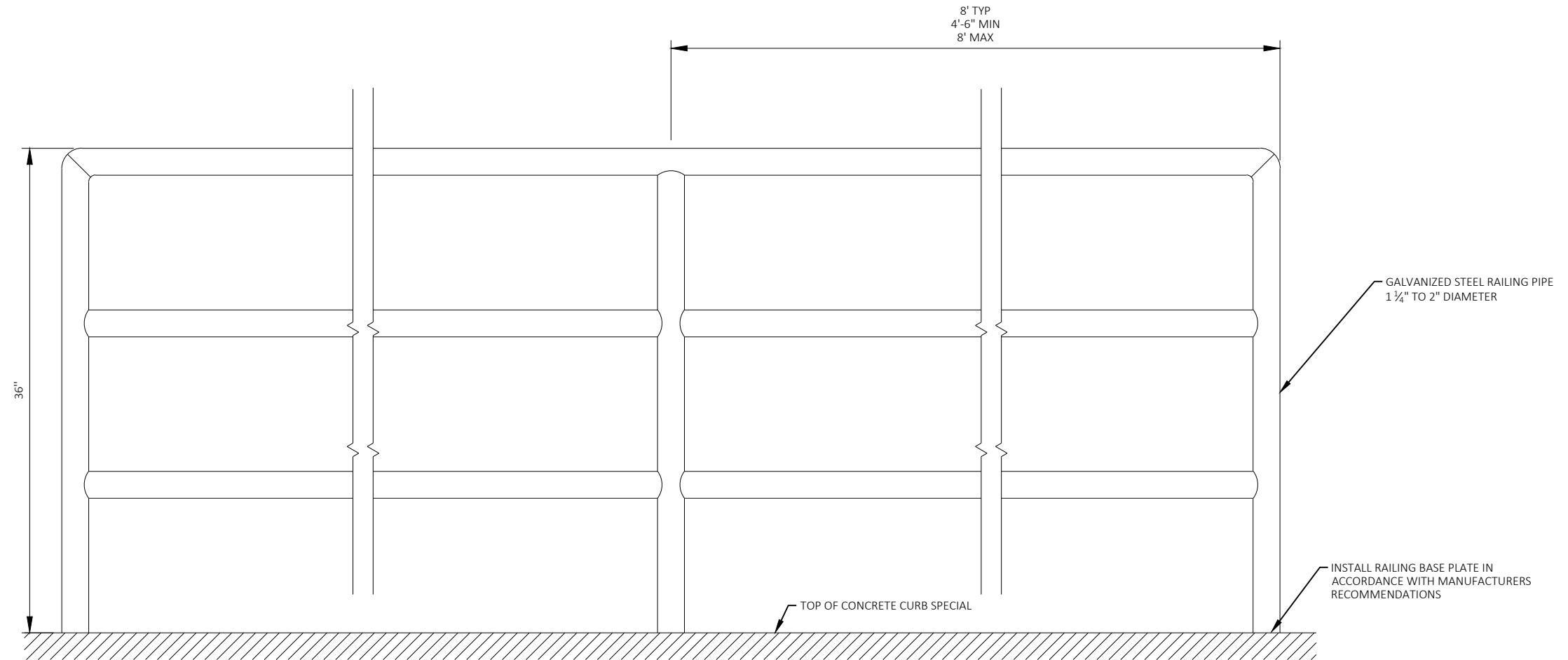


CROSSWALK CLOSURE WITH BERM

- SHADY LN - SE QUADRANT
- SHADY LN - NW & NE QUADRANTS
- ALLEN AVE - NW & NE QUADRANTS
- LLOYD AVE - NW & NE QUADRANTS
- SHERIDAN DR - NW & NE QUADRANTS
- THEODORE AVE - NE QUADRANT
- DARDIS AVE - NW & NE QUADRANTS
- HIAWATHA AVE - NW & NE QUADRANTS
- FLEET AVE - NW & NE QUADRANTS

- NOTES:
1. ALL RAILING CONSTRUCTION SHALL MEET AMERICANS WITH DISABILITIES ACT REQUIREMENTS AND ALL LOCAL BUILDING CODES.
 2. SEE CONSTRUCTION DETAIL FOR RAILING PIPE DIMENSIONS.
 3. PROVIDE SHOP DRAWINGS FOR ALL RAILING COMPONENTS TO THE ENGINEER FOR REVIEW AND APPROVAL PRIOR TO ORDERING OR FABRICATING RAILING COMPONENTS.
 4. ALL RAILINGS SHALL BE GALVANIZED STEEL.
 5. RAILINGS SHALL BE MOUNTED TO A CONCRETE FOUNDATION AND NOT THE ADJACENT SIDEWALK.
 6. ALL STEEL REINFORCEMENT IS INCIDENTAL TO THE CONCRETE.
 7. RAILINGS SHALL BE MOUNTED TO THE CONCRETE USING ANCHOR BOLTS AND A BASE PLATE. VERTICAL MEMBER OF RAILING SHALL NOT BE EMBEDDED DIRECTLY INTO THE CONCRETE.
 8. PLACE ANCHOR BOLTS FOR RAILING INTO CONCRETE IN ACCORDANCE WITH STANDARD SPEC 513. PLACE ANCHOR BOLTS DURING CONCRETE PLACEMENT UNLESS OTHERWISE APPROVED BY THE ENGINEER.
 9. GRIND AND SMOOTH ALL CONNECTIONS OF SECTIONS OF PIPE.
 10. A MINIMUM OF A 1/8" RADIUS IS REQUIRED AT ALL ANGLES OR CORNERS IN THE PIPE. THE ACTUAL RADIUS USED SHALL BE DETERMINED BY THE MANUFACTURER.





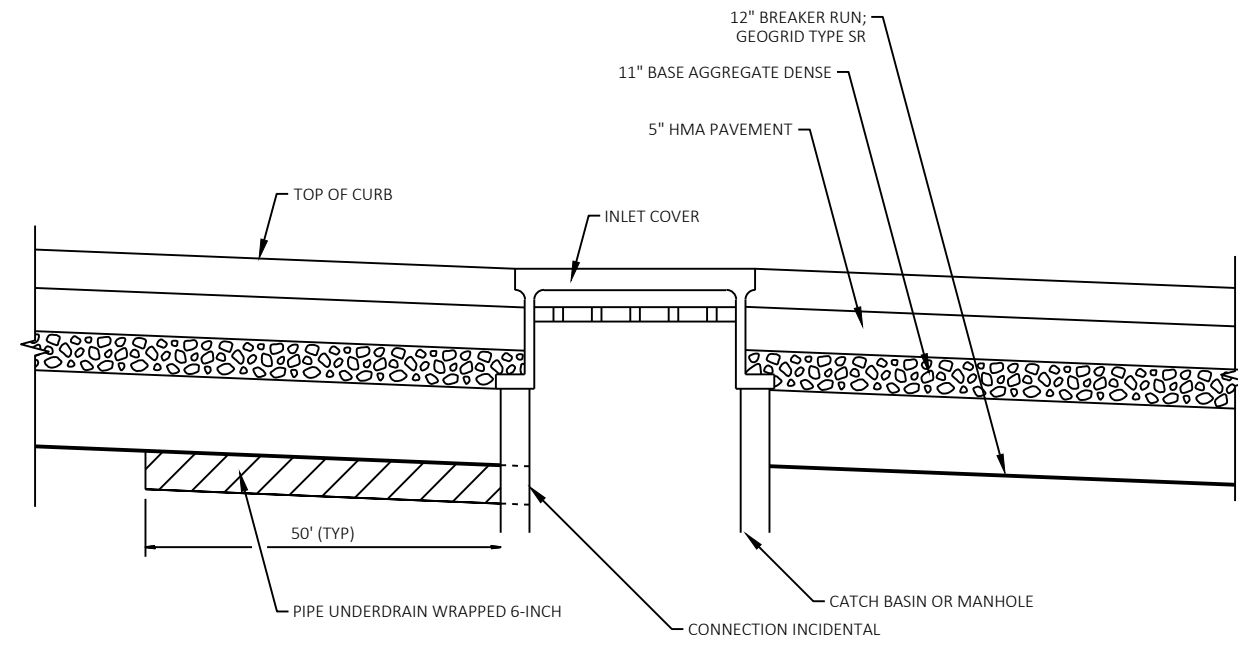
ELEVATION VIEW

NOTES:

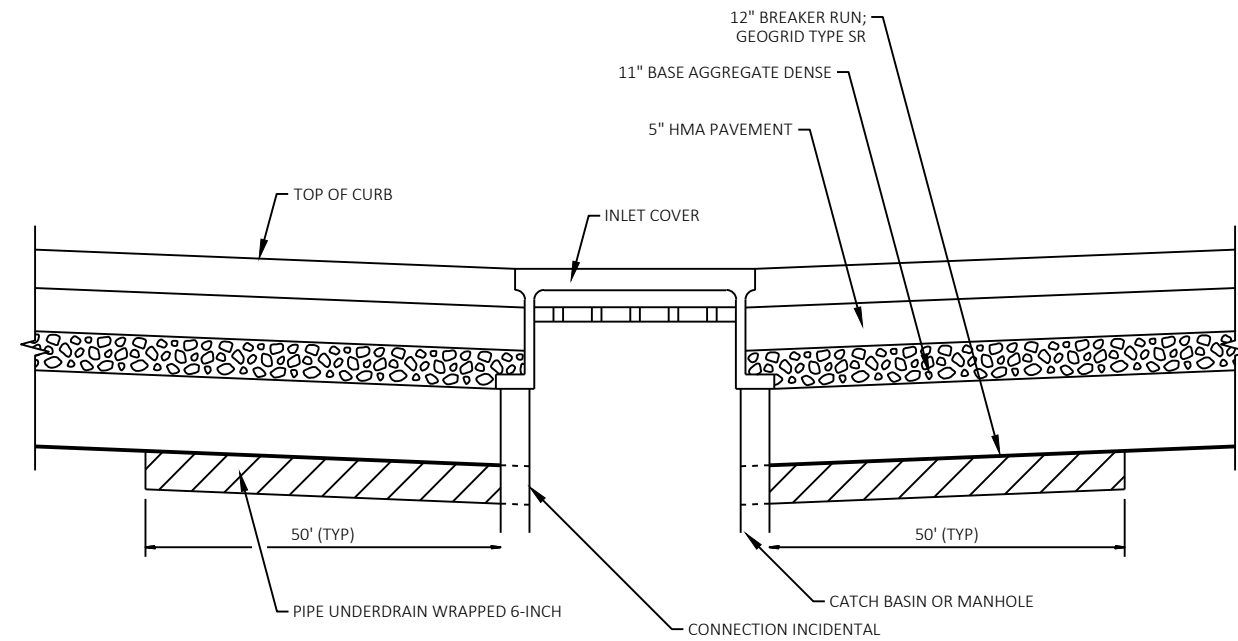
1. PROVIDE SHOP DRAWINGS FOR ALL RAILING COMPONENTS TO THE ENGINEER FOR REVIEW AND APPROVAL PRIOR TO ORDERING OR FABRICATING RAILING COMPONENTS.
2. ALL RAILINGS SHALL BE GALVANIZED STEEL.
3. RAILINGS SHALL BE MOUNTED TO THE TOP OF THE CONCRETE CURB SPECIAL.
4. RAILINGS SHALL BE MOUNTED TO CONCRETE USING ANCHOR BOLTS AND A BASE PLATE. VERTICAL MEMBER OF RAILING SHALL NOT BE EMBEDDED DIRECTLY INTO CONCRETE.
5. PLACE ANCHOR BOLTS FOR RAILING INTO CONCRETE IN ACCORDANCE WITH STANDARD SPEC 513. PLACE ANCHOR BOLTS DURING CONCRETE PLACEMENT UNLESS OTHERWISE APPROVED BY THE ENGINEER.
6. GRIND AND SMOOTH ALL CONNECTIONS OF SECTIONS OF PIPE.
7. A MINIMUM OF A 1/8" RADIUS IS REQUIRED AT ALL ANGLES OR CORNERS IN THE PIPE. THE ACTUAL RADIUS USED SHALL BE DETERMINED BY THE MANUFACTURER.

RAILING PIPE DETAIL

SEE PLAN DETAILS FOR LOCATIONS



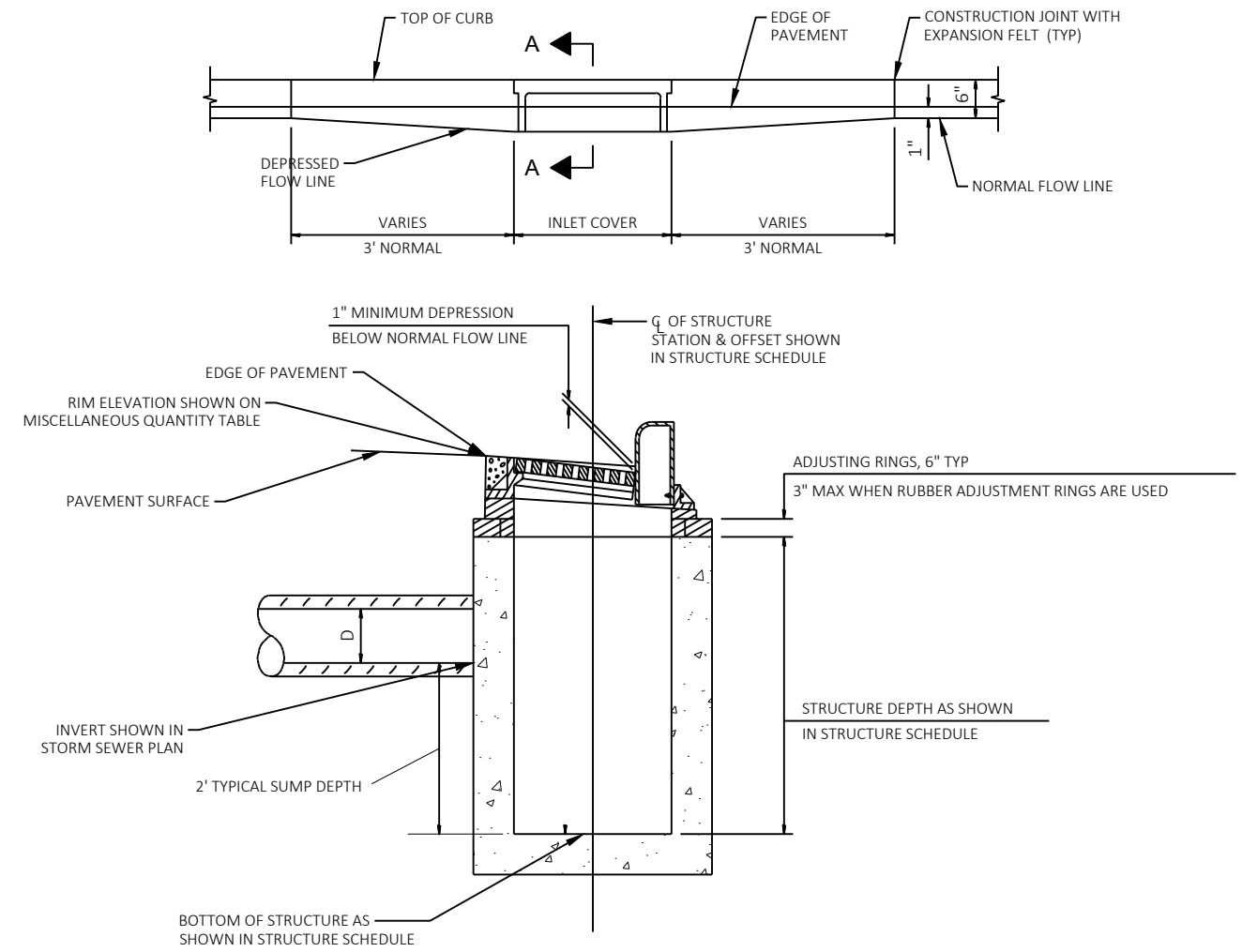
PIPE UNDERDRAIN AT ON GRADE STRUCTURES



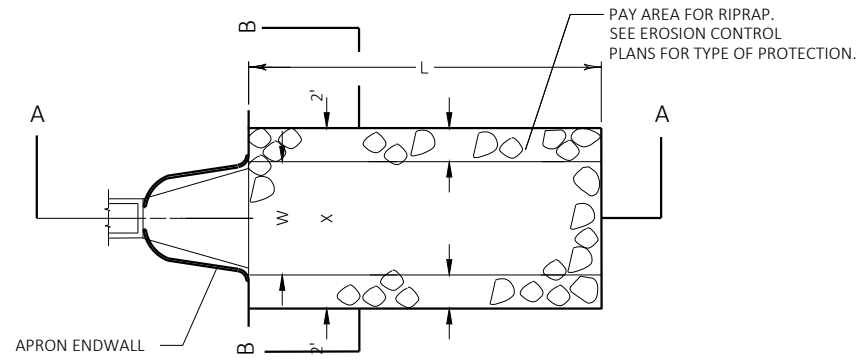
PIPE UNDERDRAIN AT LOW POINT STRUCTURES

PIPE UNDERDRAIN DETAIL

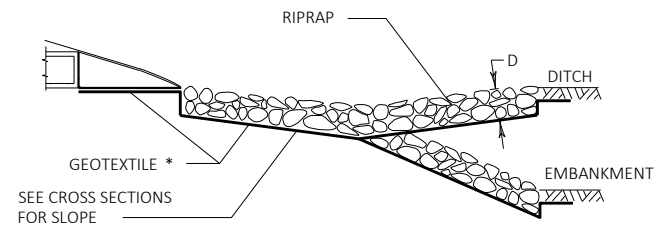
TO BE PLACED AT CATCH BASINS IN ROADWAY LOW POINTS



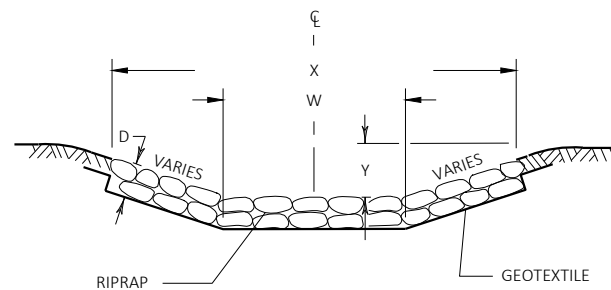
SECTION A-A
DETAIL OF CURB AND GUTTER AT CATCH BASINS



PLAN VIEW



SECTION A-A



SECTION B-B

RIPRAP AND GEOTEXTILE DETAIL
AT APRON ENDWALLS
SEE EROSION CONTROL PLAN FOR LOCATIONS

L = 3 x W (NOR) OR 10' MIN
OR AS INDICATED IN THE PLANS
OR AS DIRECTED BY THE ENGINEER

D = 24" FOR RIPRAP HEAVY

X = W+4' FOR TYPICAL CULVERT
DISCHARGE INTO DITCH
W+6' FOR CULVERT DISCHARGE
DOWN EMBANKMENT SLOPE

Y = 0' FOR TYPICAL CULVERT
DISCHARGE INTO DITCH
6" FOR CULVERT DISCHARGE
DOWN EMBANKMENT SLOPE

* TYPE HR (FOR RIPRAP HEAVY AND MEDIUM ONLY)

NOTES:

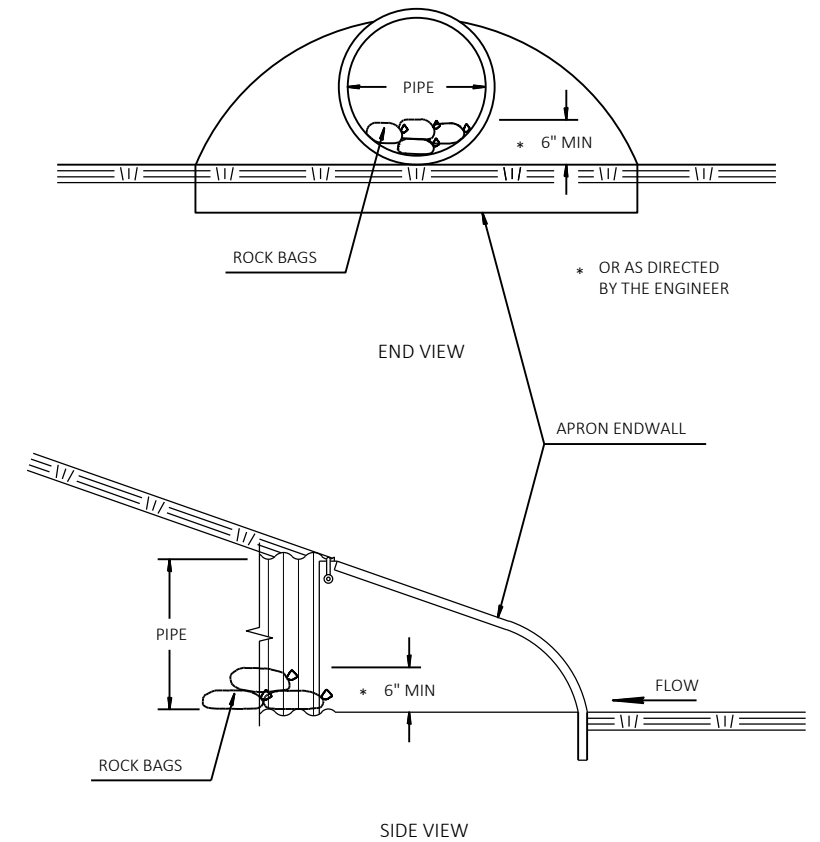
1. GEOTEXTILE SHALL EXTEND BENEATH THE ENTIRE LENGTH OF THE APRON ENDWALL SECTION. INSTALL ON PREPAVED FOUNDATION. GRADE PRIOR TO END WALL INSTALLATION.

- * 2. COMPLETE GEOTEXTILE AND RIPRAP SECTION INSTALLATION PRIOR TO STORM WATER FLOW.

ESTIMATED BAG SIZE =
18" X 12" X 6"

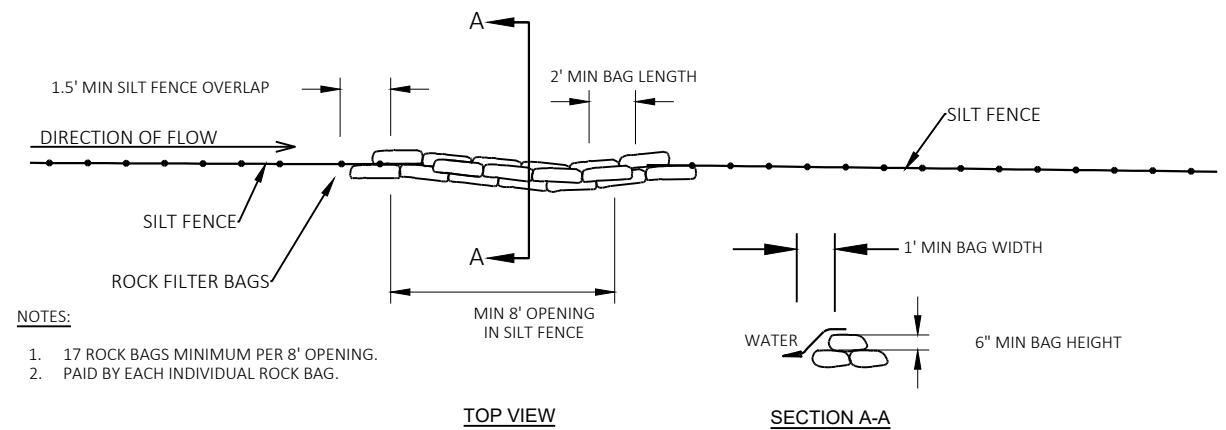
PIPE SIZE	ESTIMATED NUMBER OF BAGS
12"	1
15"	2
18"	2
24"	3
30"	5
36"	7
42"	7
48"	10
54"	10
60"	13
66"	14
72"	16
17"X13"	2
30"X19"	5
38"X24"	7
45"X29"	10
53"X34"	10
60"X38"	13
76"X48"	18

CULVERT PIPE CHECKS



CULVERT PIPE ENDS

ROCK BAGS



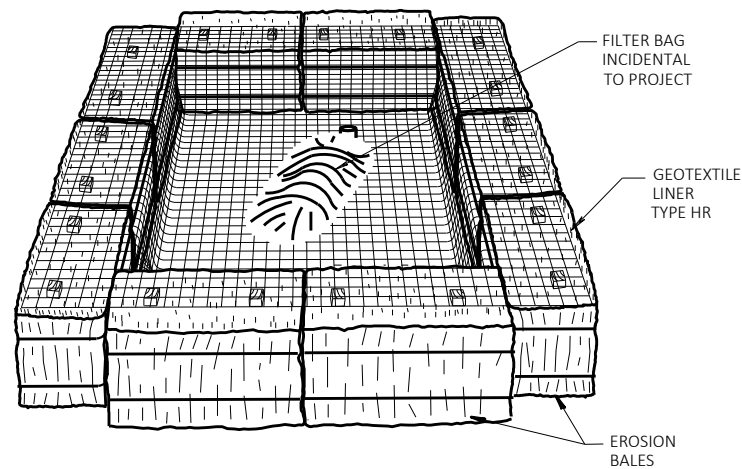
NOTES:

1. 17 ROCK BAGS MINIMUM PER 8' OPENING.
2. PAID BY EACH INDIVIDUAL ROCK BAG.

TOP VIEW

SECTION A-A

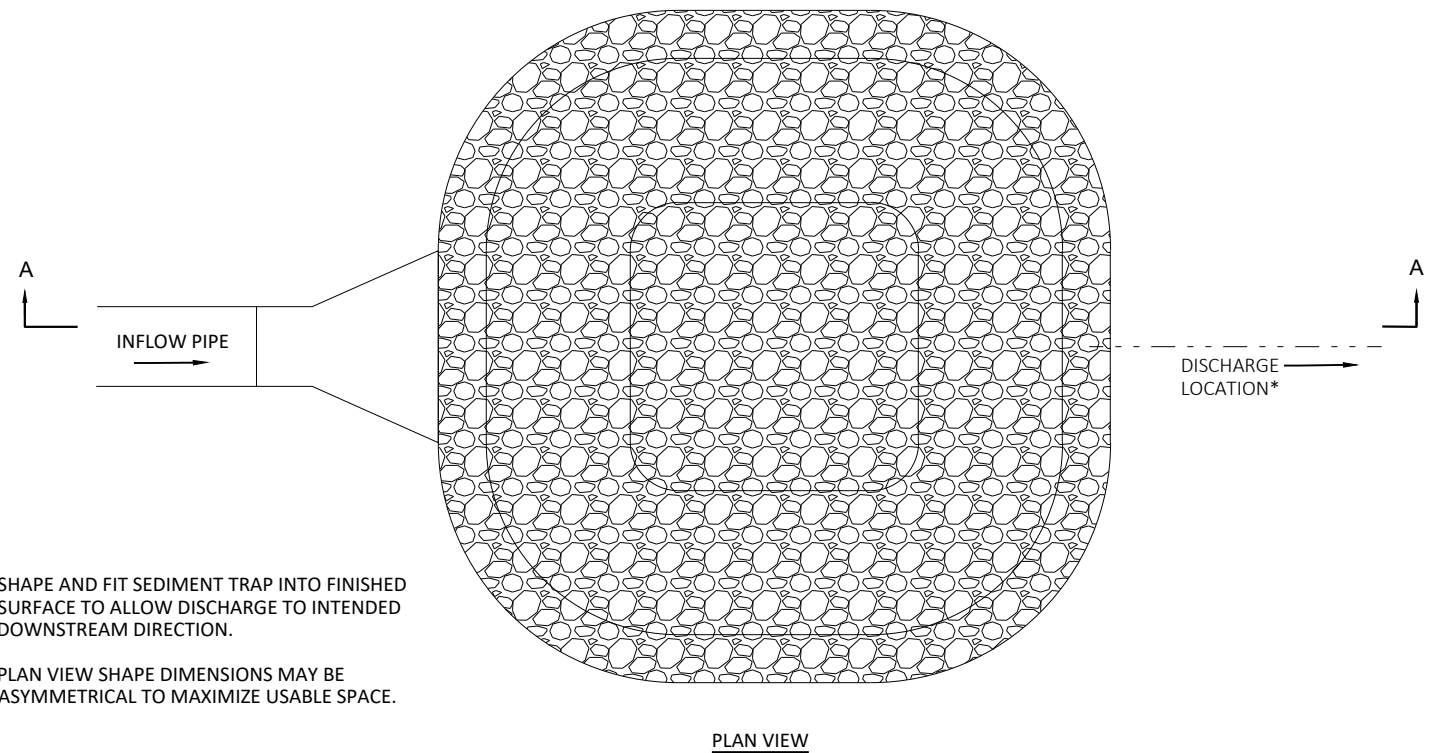
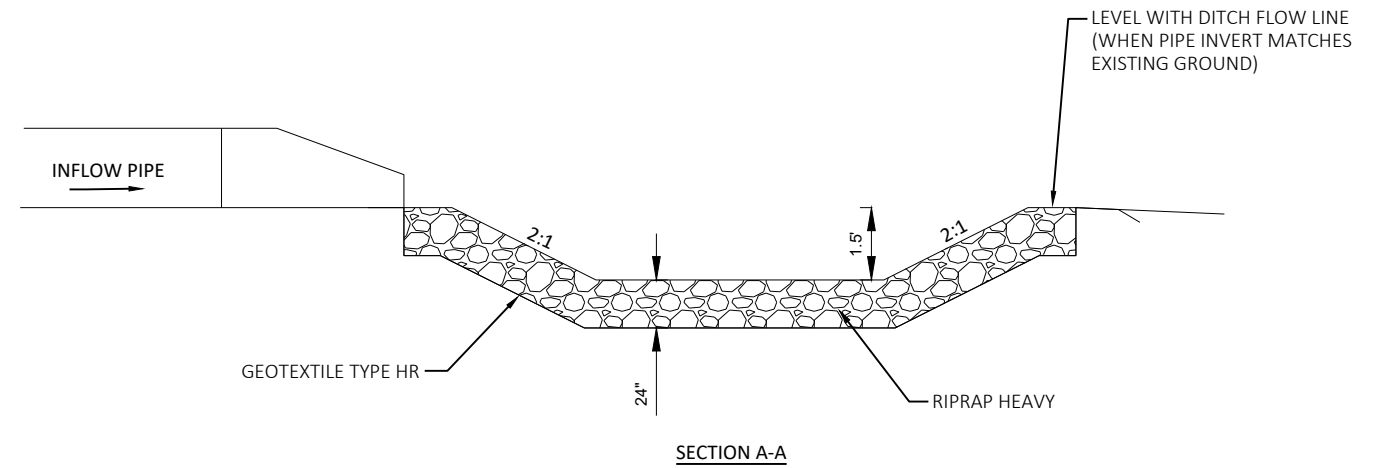
ROCK BAGS USED FOR SILT FENCE RELIEF



NOTE:
 GEOTEXTILE AND EROSION BALES WILL BE PAID UNDER THE ITEMS "GEOTEXTILE TYPE HR" AND "EROSION BALES"

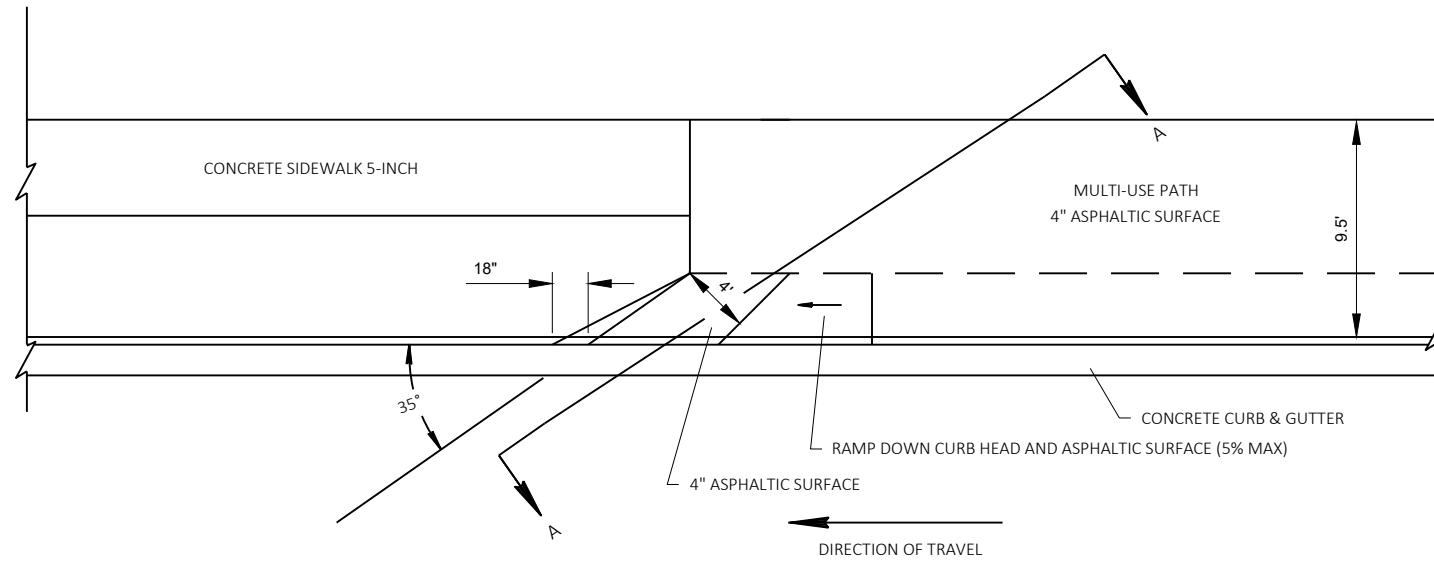
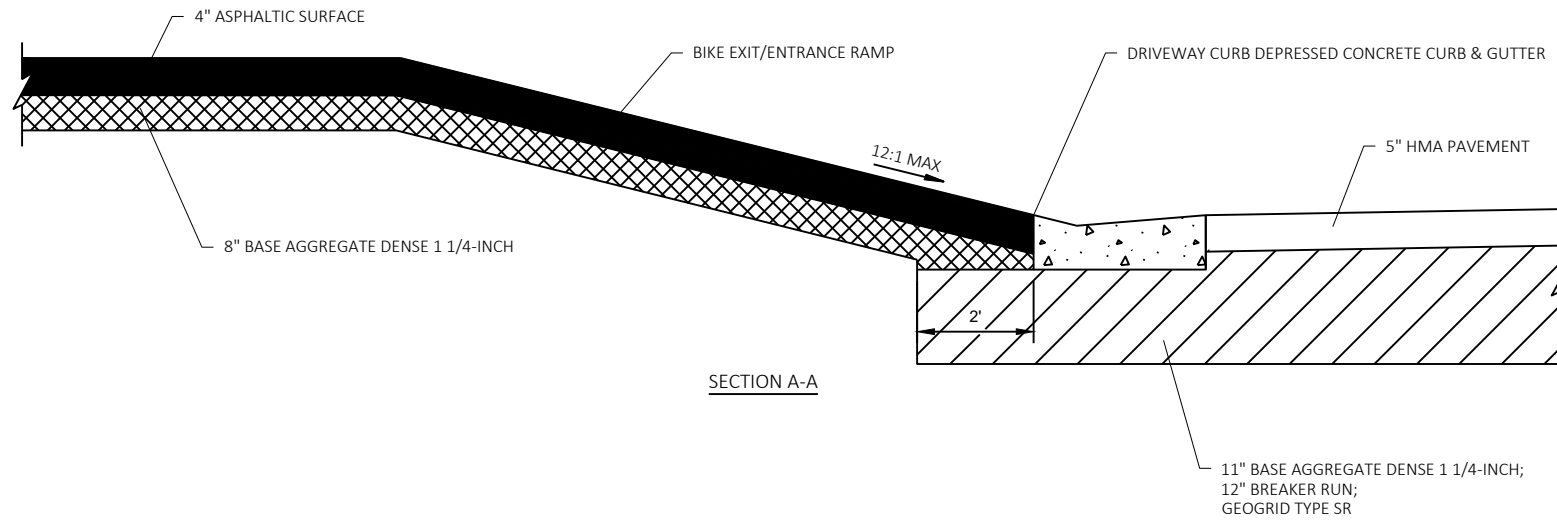
TEMPORARY SETTLING BASIN
 (SIZE TO BE DETERMINED IN FIELD AS INDICATED BELOW:)

STORAGE VOLUME (C.F.) = 16 X GPM (PUMP RATE)
 EXAMPLE:
 CONTRACTOR INDICATES PUMP CAPABLE OF 50 GPM
 HEIGHT OF BALES = 1.5 FT.
 SOLUTION:
 SV (C.F.) = 16 X 50
 SV = 800 C.F.
 800 C.F. = 533 S.F.
 1.5 FT.
 USE A 20 FT. X 27 FT. BASIN

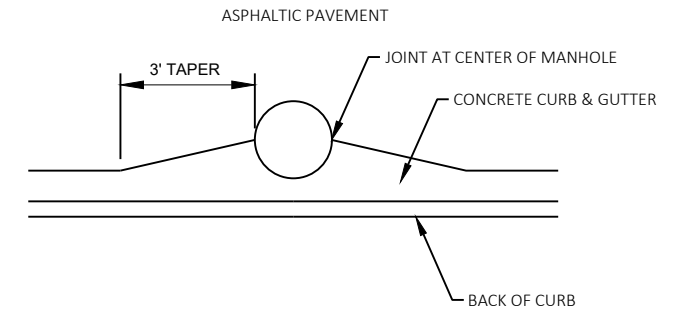


* SHAPE AND FIT SEDIMENT TRAP INTO FINISHED SURFACE TO ALLOW DISCHARGE TO INTENDED DOWNSTREAM DIRECTION.
 PLAN VIEW SHAPE DIMENSIONS MAY BE ASYMMETRICAL TO MAXIMIZE USABLE SPACE.

OUTLET PIPE SEDIMENT TRAP DETAIL
 (NOT TO SCALE)
 164+50 LT

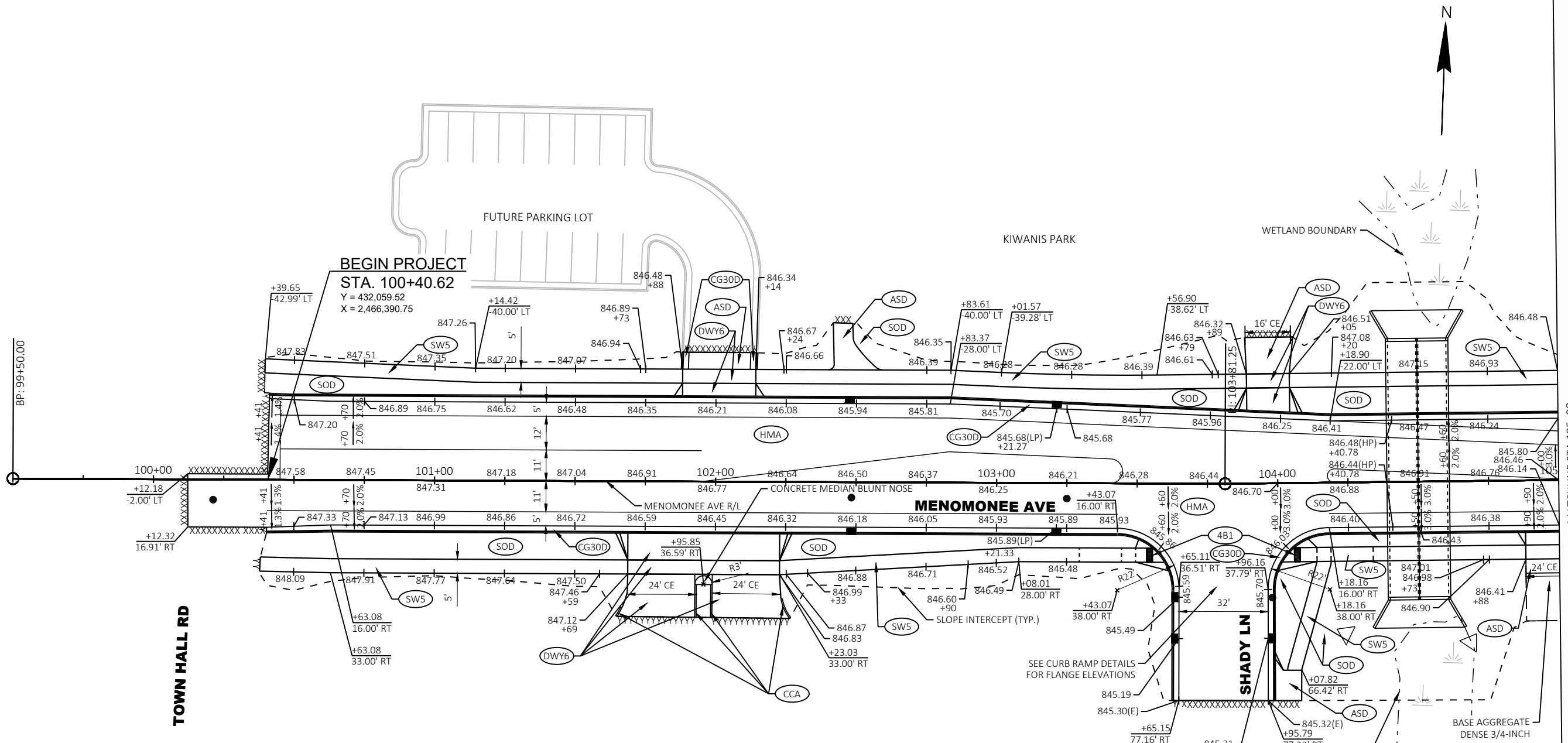


TYPICAL BIKE RAMP EXIT DETAIL
SEE PLAN DETAILS FOR LOCATIONS



TYPICAL CURB & GUTTER TRANSITION AT MANHOLE

- STA 105+29 RT
- STA 109+28 RT
- STA 111+58 RT
- STA 111+93 RT
- STA 113+78 RT
- STA 117+28 RT
- STA 120+47 RT



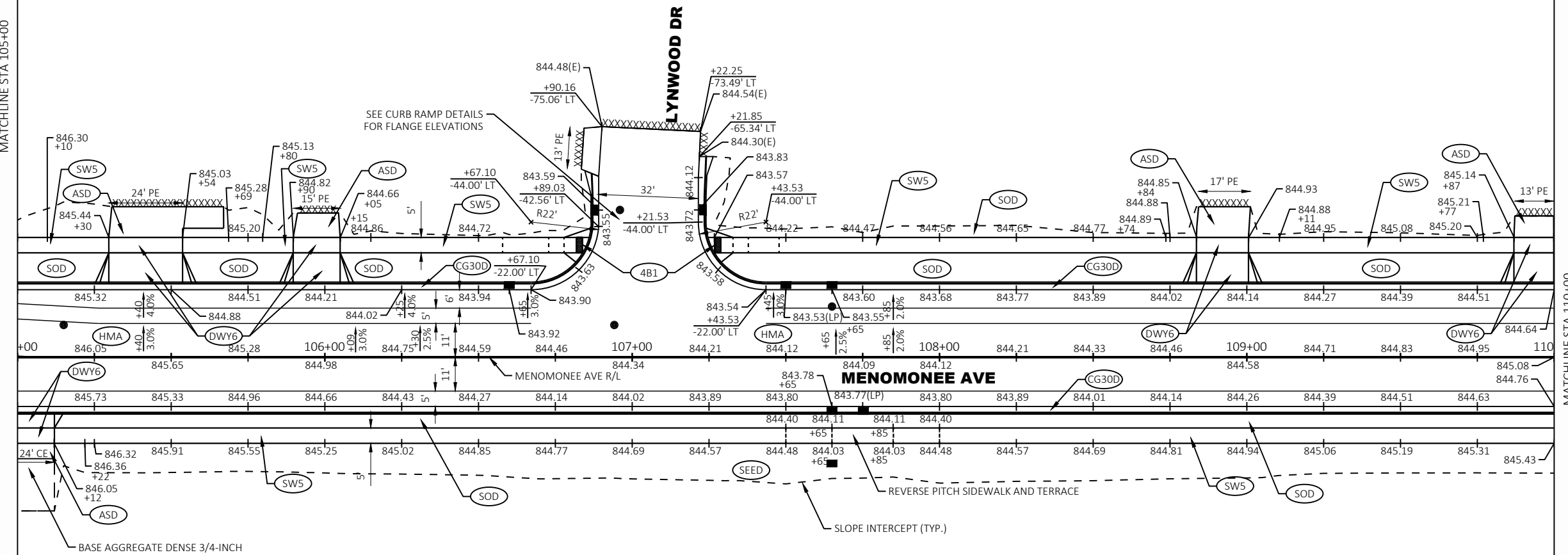
LEGEND

- | | | | |
|---------|---|-----------|-----------------------------------|
| (HMA) | 5" HMA PAVEMENT | (2) | CURB RAMP TYPE 2 |
| (SW5) | CONCRETE SIDEWALK 5-INCH | (2M) | CURB RAMP TYPE 2 - MODIFIED |
| (DWY6) | CONCRETE DRIVEWAY 6-INCH | (4B) | CURB RAMP TYPE 4B |
| (ASD) | ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES | (4B1) | CURB RAMP TYPE 4B1 |
| (CG18D) | CONCRETE CURB & GUTTER 18-INCH TYPE D | (4B1M) | CURB RAMP TYPE 4B1 - MODIFIED |
| (CG24D) | CONCRETE CURB & GUTTER 24-INCH TYPE D | XXXXXX | SAWING ASPHALT |
| (CG30D) | CONCRETE CURB & GUTTER 30-INCH TYPE D | ▲▲▲▲▲ | SAWING CONCRETE |
| (PED) | CONCRETE CURB PEDESTRIAN SPECIAL | XXX.XX | PAVEMENT OR CURB FLANGE ELEVATION |
| (CCA) | CONCRETE CURB TYPE A | XXX.XX(E) | EXISTING ELEVATION |
| (CCD) | CONCRETE CURB TYPE D | (HP) | HIGH POINT |
| (SOD) | TOPSOIL, FERTILIZER TYPE B, SOD LAWN | (LP) | LOW POINT |
| (SEED) | SEEDING MIXTURE NO. 40, FERTILIZER TYPE B, SALVAGED TOPSOIL, AND MULCH OR EROSION MAT | | |



MATCHLINE STA 105+00

MATCHLINE STA 110+00



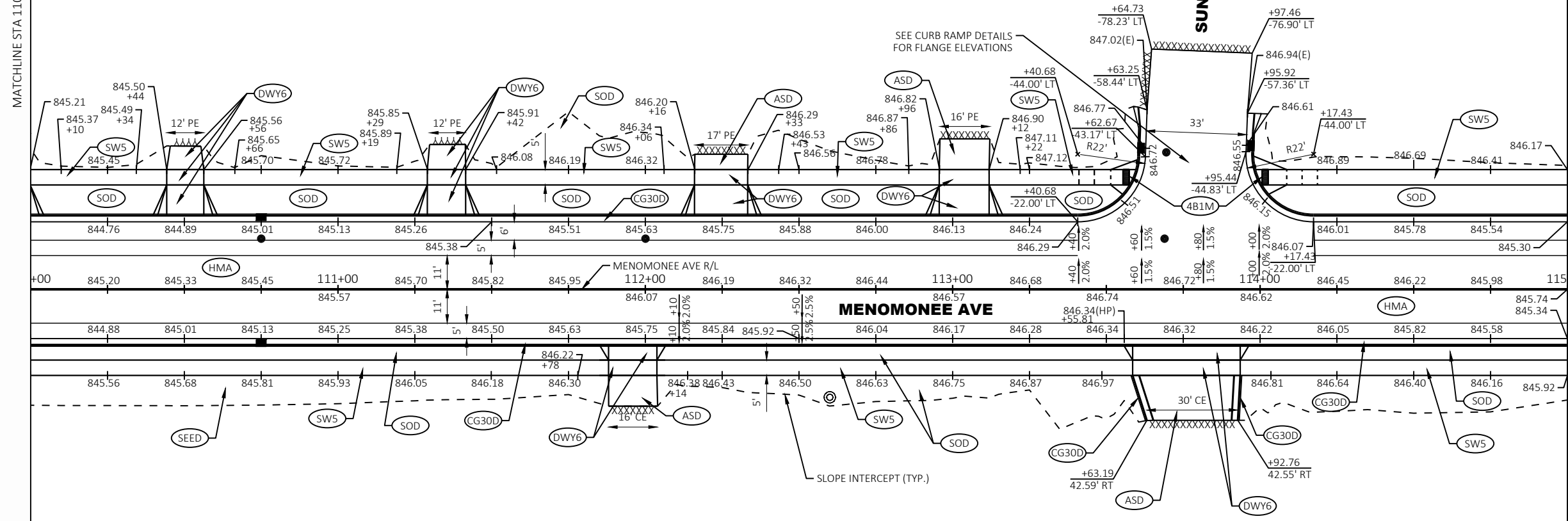
LEGEND

- | | |
|--|--|
| (HMA) 5" HMA PAVEMENT | (2) CURB RAMP TYPE 2 |
| (SW5) CONCRETE SIDEWALK 5-INCH | (2M) CURB RAMP TYPE 2 - MODIFIED |
| (DWY6) CONCRETE DRIVEWAY 6-INCH | (4B) CURB RAMP TYPE 4B |
| (ASD) ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES | (4B1) CURB RAMP TYPE 4B1 |
| (CG18D) CONCRETE CURB & GUTTER 18-INCH TYPE D | (4B1M) CURB RAMP TYPE 4B1 - MODIFIED |
| (CG24D) CONCRETE CURB & GUTTER 24-INCH TYPE D | XXXXXX SAWING ASPHALT |
| (CG30D) CONCRETE CURB & GUTTER 30-INCH TYPE D | AAAAAA SAWING CONCRETE |
| (PED) CONCRETE CURB PEDESTRIAN SPECIAL | XXX.XX PAVEMENT OR CURB FLANGE ELEVATION |
| (CCA) CONCRETE CURB TYPE A | XXX.XX(E) EXISTING ELEVATION |
| (CCD) CONCRETE CURB TYPE D | (HP) HIGH POINT |
| (SOD) TOPSOIL, FERTILIZER TYPE B, SOD LAWN | (LP) LOW POINT |
| (SEED) SEEDING MIXTURE NO. 40, FERTILIZER TYPE B, SALVAGED TOPSOIL, AND MULCH OR EROSION MAT | |



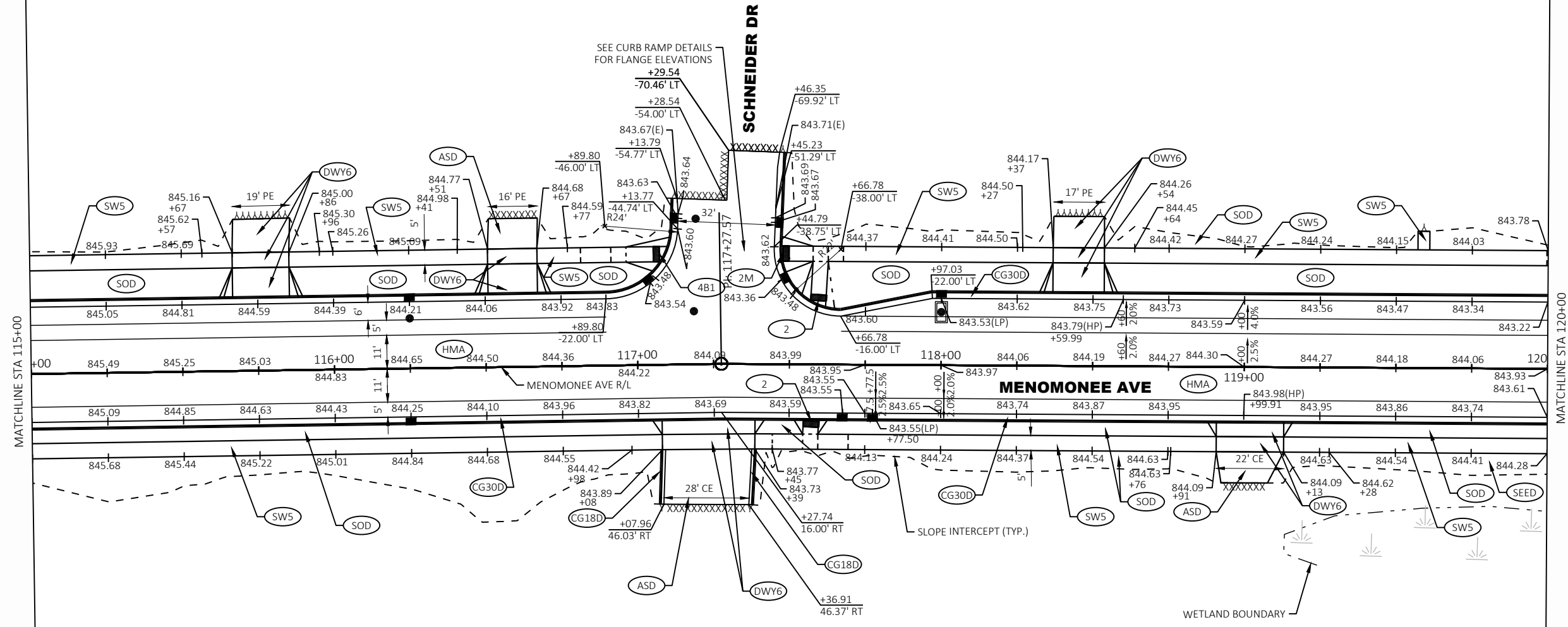
MATCHLINE STA 110+00

MATCHLINE STA 115+00



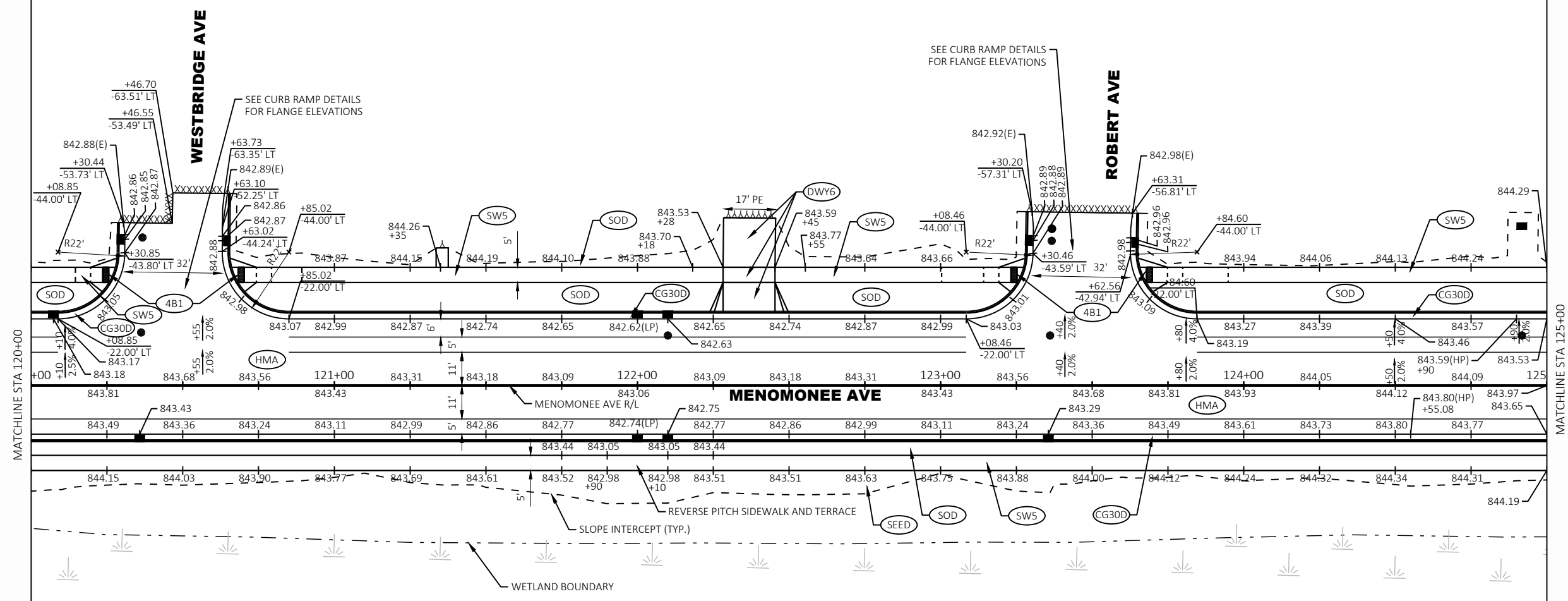
LEGEND

- (HMA) 5" HMA PAVEMENT
- (SW5) CONCRETE SIDEWALK 5-INCH
- (DWY6) CONCRETE DRIVEWAY 6-INCH
- (ASD) ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES
- (CG18D) CONCRETE CURB & GUTTER 18-INCH TYPE D
- (CG24D) CONCRETE CURB & GUTTER 24-INCH TYPE D
- (CG30D) CONCRETE CURB & GUTTER 30-INCH TYPE D
- (PED) CONCRETE CURB PEDESTRIAN SPECIAL
- (CCA) CONCRETE CURB TYPE A
- (CCD) CONCRETE CURB TYPE D
- (SOD) TOPSOIL, FERTILIZER TYPE B, SOD LAWN
- (SEED) SEEDING MIXTURE NO. 40, FERTILIZER TYPE B, SALVAGED TOPSOIL, AND MULCH OR EROSION MAT
- (2) CURB RAMP TYPE 2
- (2M) CURB RAMP TYPE 2 - MODIFIED
- (4B) CURB RAMP TYPE 4B
- (4B1) CURB RAMP TYPE 4B1
- (4B1M) CURB RAMP TYPE 4B1 - MODIFIED
- XXXXXX SAWING ASPHALT
- AAAAAA SAWING CONCRETE
- XXX.XX PAVEMENT OR CURB FLANGE ELEVATION
- XXX.XX(E) EXISTING ELEVATION
- (HP) HIGH POINT
- (LP) LOW POINT



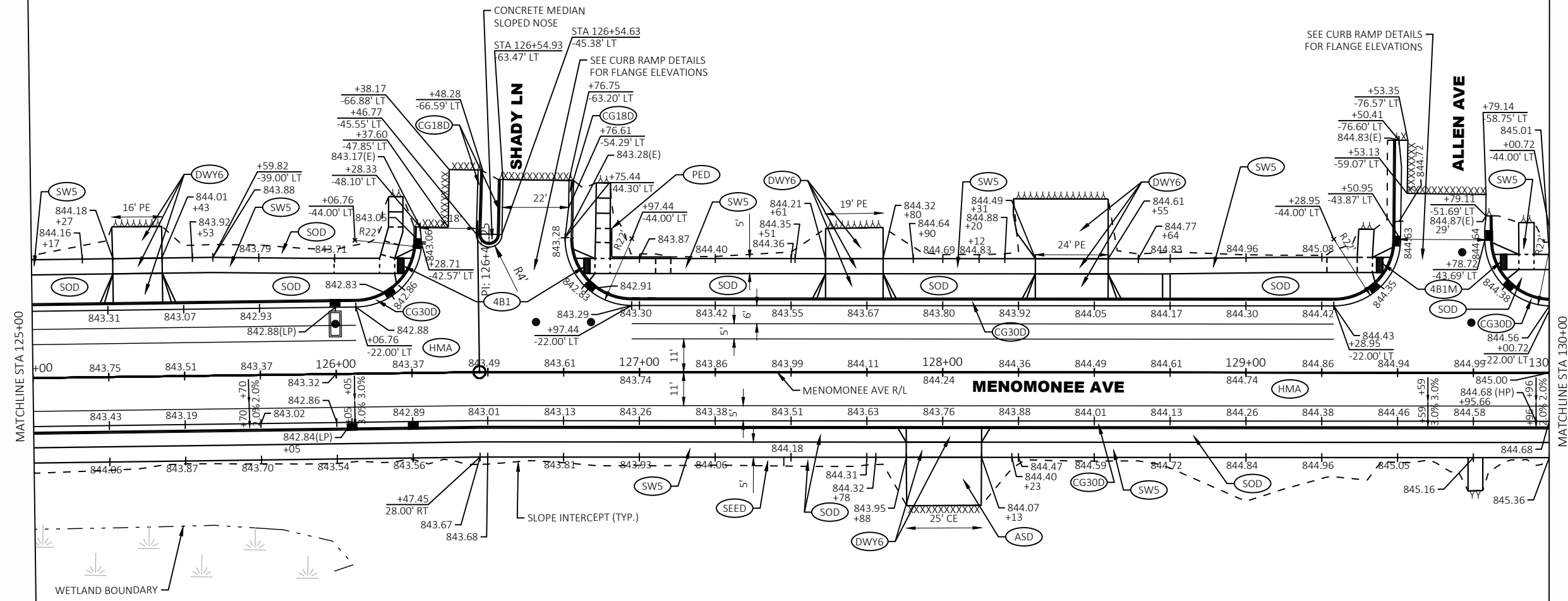
LEGEND

- (HMA) 5" HMA PAVEMENT
- (SW5) CONCRETE SIDEWALK 5-INCH
- (DWY6) CONCRETE DRIVEWAY 6-INCH
- (ASD) ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES
- (CG18D) CONCRETE CURB & GUTTER 18-INCH TYPE D
- (CG24D) CONCRETE CURB & GUTTER 24-INCH TYPE D
- (CG30D) CONCRETE CURB & GUTTER 30-INCH TYPE D
- (PED) CONCRETE CURB PEDESTRIAN SPECIAL
- (CCA) CONCRETE CURB TYPE A
- (CCD) CONCRETE CURB TYPE D
- (SOD) TOPSOIL, FERTILIZER TYPE B, SOD LAWN
- (SEED) SEEDING MIXTURE NO. 40, FERTILIZER TYPE B, SALVAGED TOPSOIL, AND MULCH OR EROSION MAT
- (2) CURB RAMP TYPE 2
- (2M) CURB RAMP TYPE 2 - MODIFIED
- (4B) CURB RAMP TYPE 4B
- (4B1) CURB RAMP TYPE 4B1
- (4B1M) CURB RAMP TYPE 4B1 - MODIFIED
- XXXXXX SAWING ASPHALT
- AAAAAA SAWING CONCRETE
- XXX.XX PAVEMENT OR CURB FLANGE ELEVATION
- XXX.XX(E) EXISTING ELEVATION
- (HP) HIGH POINT
- (LP) LOW POINT



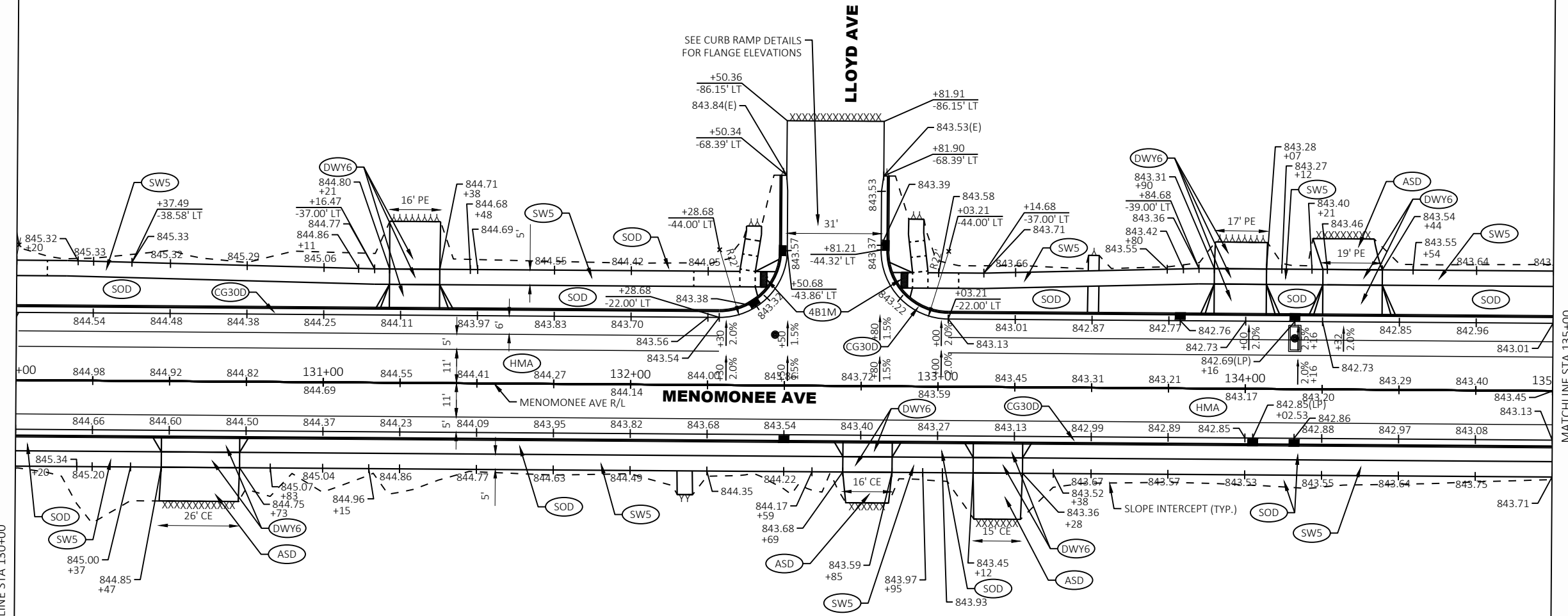
LEGEND

- | | | | |
|-------|---|-----------|-----------------------------------|
| HMA | 5" HMA PAVEMENT | 2 | CURB RAMP TYPE 2 |
| SW5 | CONCRETE SIDEWALK 5-INCH | 2M | CURB RAMP TYPE 2 - MODIFIED |
| DWY6 | CONCRETE DRIVEWAY 6-INCH | 4B | CURB RAMP TYPE 4B |
| ASD | ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES | 4B1 | CURB RAMP TYPE 4B1 |
| CG18D | CONCRETE CURB & GUTTER 18-INCH TYPE D | 4B1M | CURB RAMP TYPE 4B1 - MODIFIED |
| CG24D | CONCRETE CURB & GUTTER 24-INCH TYPE D | XXXXXX | SAWING ASPHALT |
| CG30D | CONCRETE CURB & GUTTER 30-INCH TYPE D | AAAAAA | SAWING CONCRETE |
| PED | CONCRETE CURB PEDESTRIAN SPECIAL | XXX.XX | PAVEMENT OR CURB FLANGE ELEVATION |
| CCA | CONCRETE CURB TYPE A | XXX.XX(E) | EXISTING ELEVATION |
| CCD | CONCRETE CURB TYPE D | (HP) | HIGH POINT |
| SOD | TOPSOIL, FERTILIZER TYPE B, SOD LAWN | (LP) | LOW POINT |
| SEED | SEEDING MIXTURE NO. 40, FERTILIZER TYPE B, SALVAGED TOPSOIL, AND MULCH OR EROSION MAT | | |



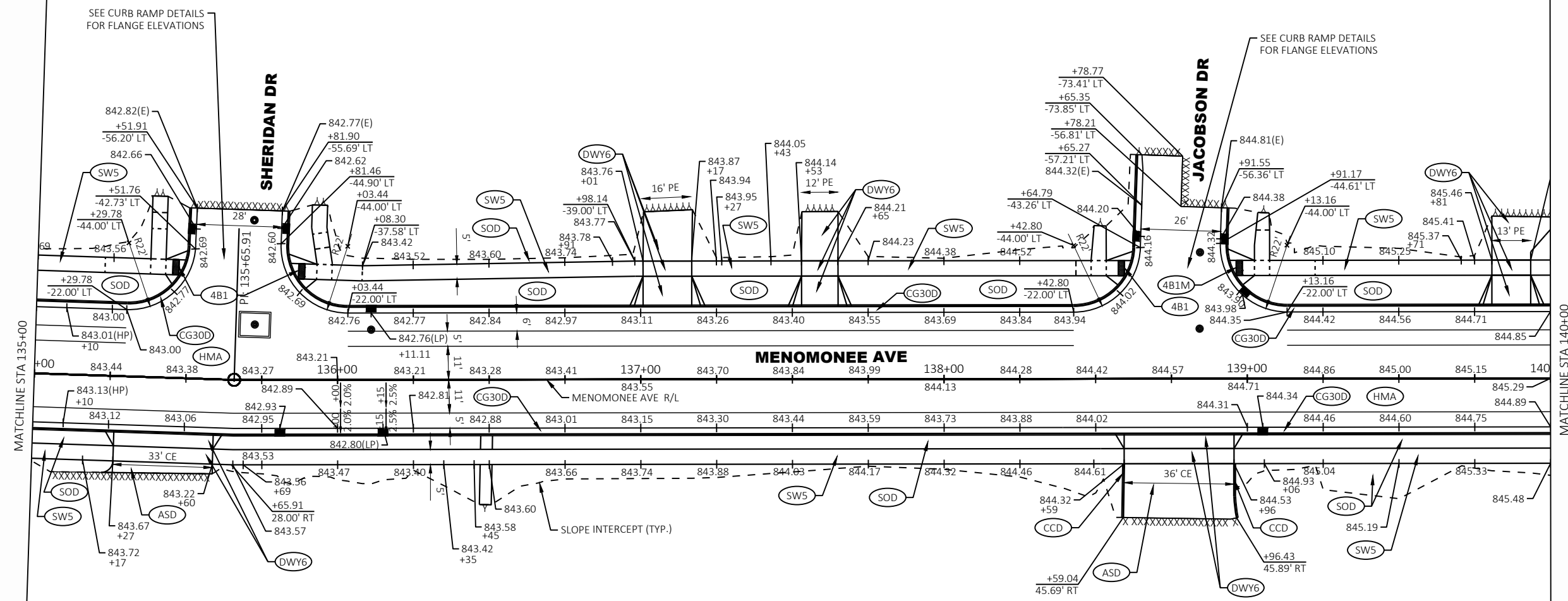
LEGEND

- | | |
|--|--|
| (HMA) 5" HMA PAVEMENT | (2) CURB RAMP TYPE 2 |
| (SW5) CONCRETE SIDEWALK 5-INCH | (2M) CURB RAMP TYPE 2 - MODIFIED |
| (DWY6) CONCRETE DRIVEWAY 6-INCH | (4B) CURB RAMP TYPE 4B |
| (ASD) ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES | (4B1) CURB RAMP TYPE 4B1 |
| (CG18D) CONCRETE CURB & GUTTER 18-INCH TYPE D | (4B1M) CURB RAMP TYPE 4B1 - MODIFIED |
| (CG24D) CONCRETE CURB & GUTTER 24-INCH TYPE D | XXXXXX SAWING ASPHALT |
| (CG30D) CONCRETE CURB & GUTTER 30-INCH TYPE D | ▲▲▲▲▲ SAWING CONCRETE |
| (PED) CONCRETE CURB PEDESTRIAN SPECIAL | XXX.XX PAVEMENT OR CURB FLANGE ELEVATION |
| (CCA) CONCRETE CURB TYPE A | XXX.XX(E) EXISTING ELEVATION |
| (CCD) CONCRETE CURB TYPE D | (HP) HIGH POINT |
| (SOD) TOPSOIL, FERTILIZER TYPE B, SOD LAWN | (LP) LOW POINT |
| (SEED) SEEDING MIXTURE NO. 40, FERTILIZER TYPE B, SALVAGED TOPSOIL, AND MULCH OR EROSION MAT | |



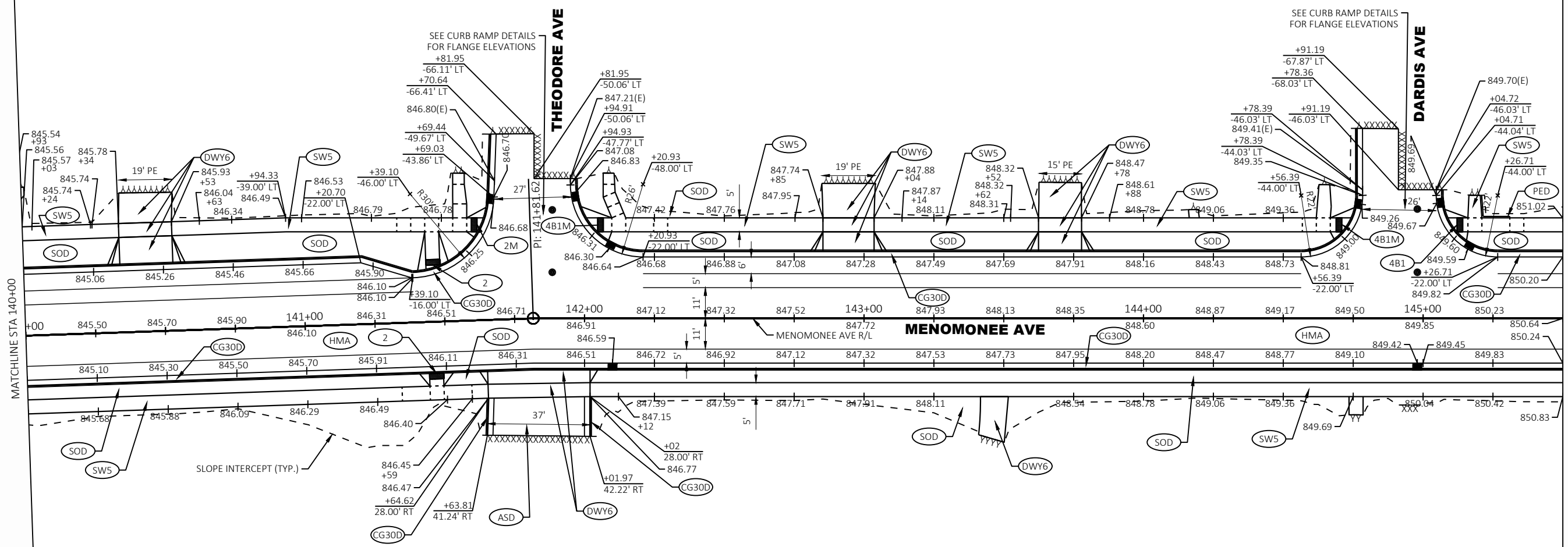
LEGEND

- | | |
|--|--|
| (HMA) 5" HMA PAVEMENT | (2) CURB RAMP TYPE 2 |
| (SW5) CONCRETE SIDEWALK 5-INCH | (2M) CURB RAMP TYPE 2 - MODIFIED |
| (DWY6) CONCRETE DRIVEWAY 6-INCH | (4B) CURB RAMP TYPE 4B |
| (ASD) ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES | (4B1) CURB RAMP TYPE 4B1 |
| (CG18D) CONCRETE CURB & GUTTER 18-INCH TYPE D | (4B1M) CURB RAMP TYPE 4B1 - MODIFIED |
| (CG24D) CONCRETE CURB & GUTTER 24-INCH TYPE D | XXXXXX SAWING ASPHALT |
| (CG30D) CONCRETE CURB & GUTTER 30-INCH TYPE D | AAAAAA SAWING CONCRETE |
| (PED) CONCRETE CURB PEDESTRIAN SPECIAL | XXX.XX PAVEMENT OR CURB FLANGE ELEVATION |
| (CCA) CONCRETE CURB TYPE A | XXX.XX(E) EXISTING ELEVATION |
| (CCD) CONCRETE CURB TYPE D | (HP) HIGH POINT |
| (SOD) TOPSOIL, FERTILIZER TYPE B, SOD LAWN | (LP) LOW POINT |
| (SEED) SEEDING MIXTURE NO. 40, FERTILIZER TYPE B, SALVAGED TOPSOIL, AND MULCH OR EROSION MAT | |



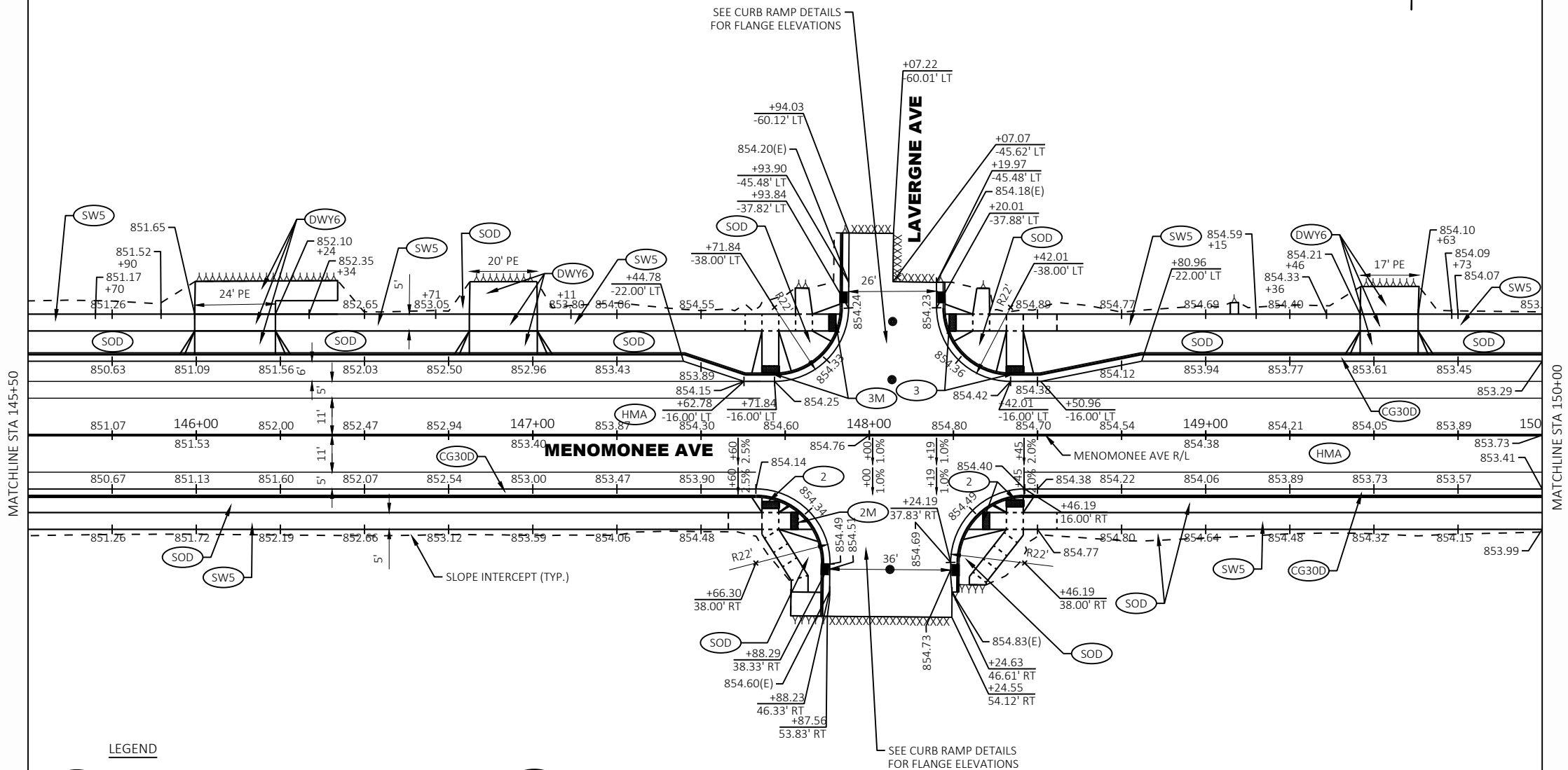
LEGEND

(HMA)	5" HMA PAVEMENT	(2)	CURB RAMP TYPE 2
(SW5)	CONCRETE SIDEWALK 5-INCH	(2M)	CURB RAMP TYPE 2 - MODIFIED
(DWY6)	CONCRETE DRIVEWAY 6-INCH	(4B)	CURB RAMP TYPE 4B
(ASD)	ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES	(4B1)	CURB RAMP TYPE 4B1
(CG18D)	CONCRETE CURB & GUTTER 18-INCH TYPE D	(4B1M)	CURB RAMP TYPE 4B1 - MODIFIED
(CG24D)	CONCRETE CURB & GUTTER 24-INCH TYPE D	XXXXXX	SAWING ASPHALT
(CG30D)	CONCRETE CURB & GUTTER 30-INCH TYPE D	AAAAAA	SAWING CONCRETE
(PED)	CONCRETE CURB PEDESTRIAN SPECIAL	XXX.XX	PAVEMENT OR CURB FLANGE ELEVATION
(CCA)	CONCRETE CURB TYPE A	XXX.XX(E)	EXISTING ELEVATION
(CCD)	CONCRETE CURB TYPE D	(HP)	HIGH POINT
(SOD)	TOPSOIL, FERTILIZER TYPE B, SOD LAWN	(LP)	LOW POINT
(SEED)	SEEDING MIXTURE NO. 40, FERTILIZER TYPE B, SALVAGED TOPSOIL, AND MULCH OR EROSION MAT		

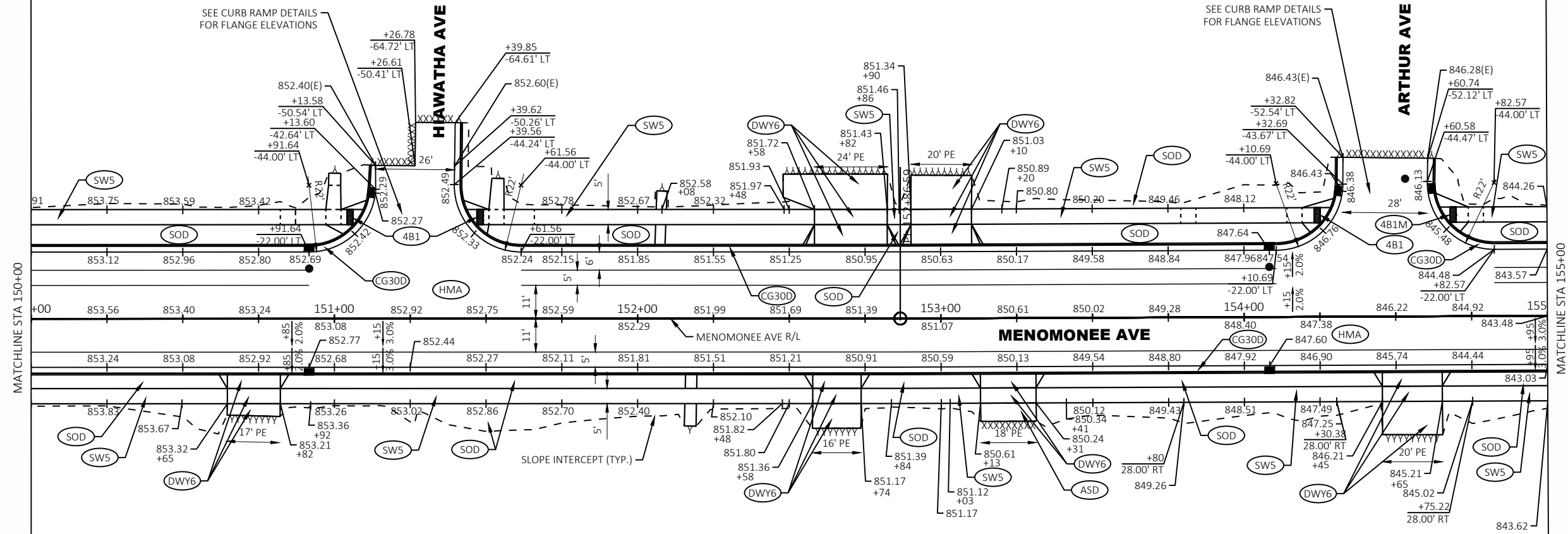


LEGEND

- (HMA) 5" HMA PAVEMENT
- (SW5) CONCRETE SIDEWALK 5-INCH
- (DWY6) CONCRETE DRIVEWAY 6-INCH
- (ASD) ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES
- (CG18D) CONCRETE CURB & GUTTER 18-INCH TYPE D
- (CG24D) CONCRETE CURB & GUTTER 24-INCH TYPE D
- (CG30D) CONCRETE CURB & GUTTER 30-INCH TYPE D
- (PED) CONCRETE CURB PEDESTRIAN SPECIAL
- (CCA) CONCRETE CURB TYPE A
- (CCD) CONCRETE CURB TYPE D
- (SOD) TOPSOIL, FERTILIZER TYPE B, SOD LAWN
- (SEED) SEEDING MIXTURE NO. 40, FERTILIZER TYPE B, SALVAGED TOPSOIL, AND MULCH OR EROSION MAT
- (2) CURB RAMP TYPE 2
- (2M) CURB RAMP TYPE 2 - MODIFIED
- (4B) CURB RAMP TYPE 4B
- (4B1) CURB RAMP TYPE 4B1
- (4B1M) CURB RAMP TYPE 4B1 - MODIFIED
- XXXXXX SAWING ASPHALT
- AAAAAA SAWING CONCRETE
- XXX.XX PAVEMENT OR CURB FLANGE ELEVATION
- XXX.XX(E) EXISTING ELEVATION
- (HP) HIGH POINT
- (LP) LOW POINT



- LEGEND**
- (HMA) 5" HMA PAVEMENT
 - (SW5) CONCRETE SIDEWALK 5-INCH
 - (DWY6) CONCRETE DRIVEWAY 6-INCH
 - (ASD) ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES
 - (CG18D) CONCRETE CURB & GUTTER 18-INCH TYPE D
 - (CG24D) CONCRETE CURB & GUTTER 24-INCH TYPE D
 - (CG30D) CONCRETE CURB & GUTTER 30-INCH TYPE D
 - (PED) CONCRETE CURB PEDESTRIAN SPECIAL
 - (CCA) CONCRETE CURB TYPE A
 - (CCD) CONCRETE CURB TYPE D
 - (SOD) TOPSOIL, FERTILIZER TYPE B, SOD LAWN
 - (SEED) SEEDING MIXTURE NO. 40, FERTILIZER TYPE B, SALVAGED TOPSOIL, AND MULCH OR EROSION MAT
 - (2) CURB RAMP TYPE 2
 - (2M) CURB RAMP TYPE 2 - MODIFIED
 - (4B) CURB RAMP TYPE 4B
 - (4B1) CURB RAMP TYPE 4B1
 - (4B1M) CURB RAMP TYPE 4B1 - MODIFIED
 - XXXXXX SAWING ASPHALT
 - AAAAAA SAWING CONCRETE
 - XXX.XX PAVEMENT OR CURB FLANGE ELEVATION
 - XXX.XX(E) EXISTING ELEVATION
 - (HP) HIGH POINT
 - (LP) LOW POINT



LEGEND

- | | |
|--|--|
| (HMA) 5" HMA PAVEMENT | (2) CURB RAMP TYPE 2 |
| (SW5) CONCRETE SIDEWALK 5-INCH | (2M) CURB RAMP TYPE 2 - MODIFIED |
| (DWY6) CONCRETE DRIVEWAY 6-INCH | (4B) CURB RAMP TYPE 4B |
| (ASD) ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES | (4B1) CURB RAMP TYPE 4B1 |
| (CG18D) CONCRETE CURB & GUTTER 18-INCH TYPE D | (4B1M) CURB RAMP TYPE 4B1 - MODIFIED |
| (CG24D) CONCRETE CURB & GUTTER 24-INCH TYPE D | XXXXXX SAWING ASPHALT |
| (CG30D) CONCRETE CURB & GUTTER 30-INCH TYPE D | AAAAAA SAWING CONCRETE |
| (PED) CONCRETE CURB PEDESTRIAN SPECIAL | XXX.XX PAVEMENT OR CURB FLANGE ELEVATION |
| (CCA) CONCRETE CURB TYPE A | XXX.XX(E) EXISTING ELEVATION |
| (CCD) CONCRETE CURB TYPE D | (HP) HIGH POINT |
| (SOD) TOPSOIL, FERTILIZER TYPE B, SOD LAWN | (LP) LOW POINT |
| (SEED) SEEDING MIXTURE NO. 40, FERTILIZER TYPE B, SALVAGED TOPSOIL, AND MULCH OR EROSION MAT | |

PROJECT NO: 2720-07-71

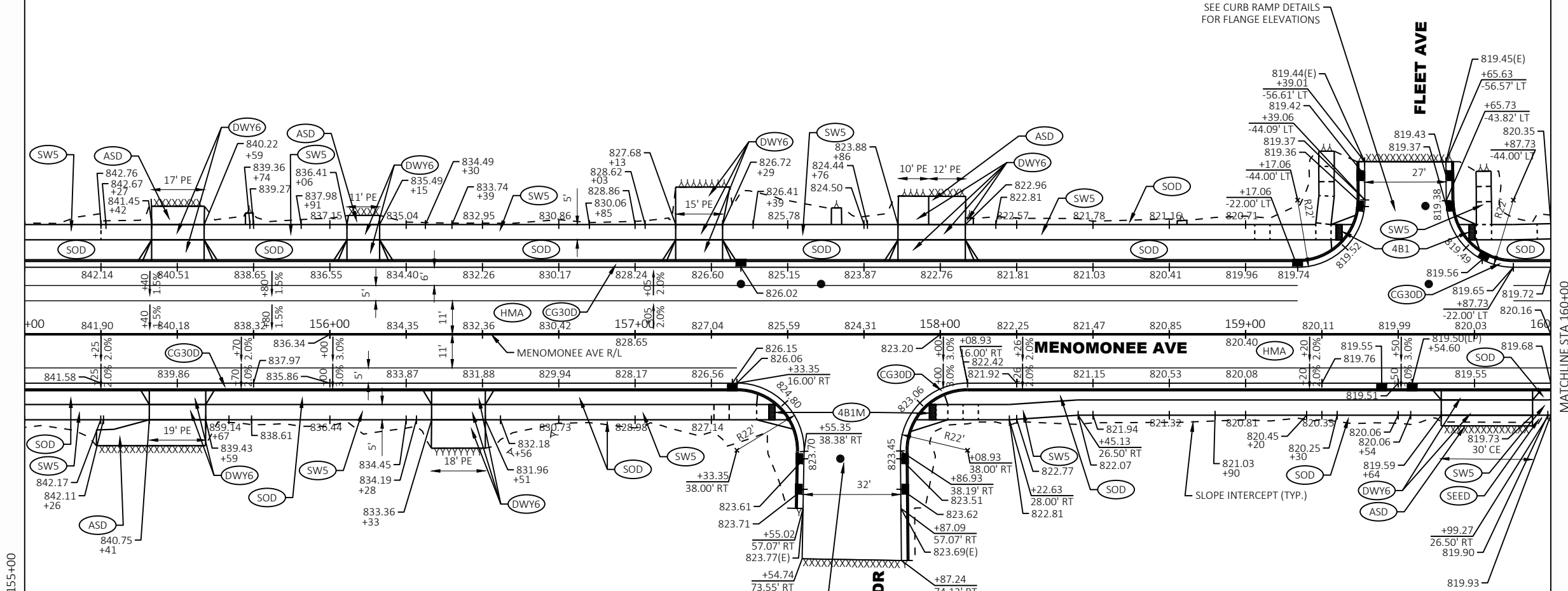
HWY: MENOMONEE AVE

COUNTY: WAUKESHA

PLAN DETAILS

SHEET

E

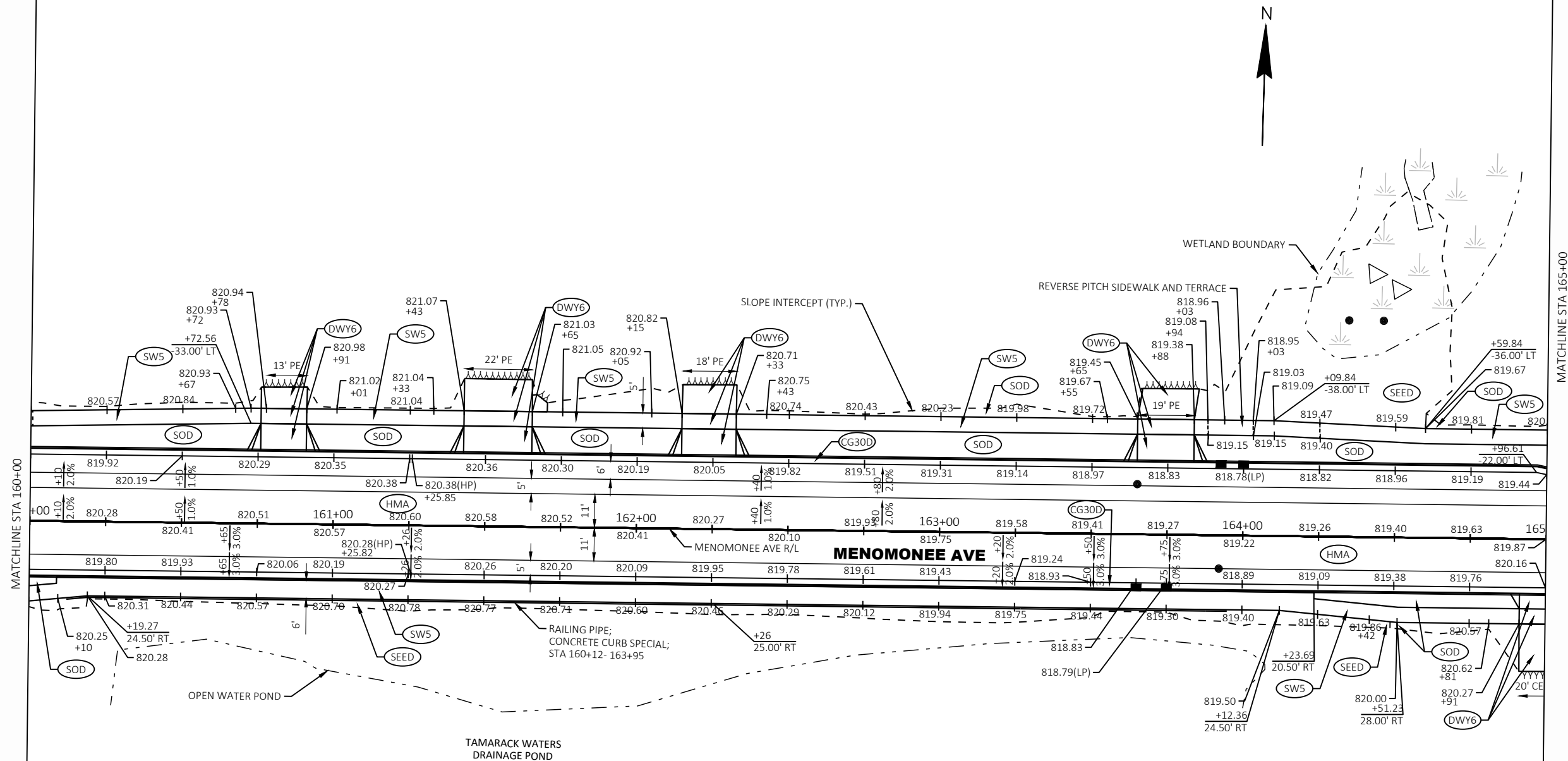


MATCHLINE STA 155+00

MATCHLINE STA 160+00

LEGEND

- | | |
|--|--|
| (HMA) 5" HMA PAVEMENT | (2) CURB RAMP TYPE 2 |
| (SW5) CONCRETE SIDEWALK 5-INCH | (2M) CURB RAMP TYPE 2 - MODIFIED |
| (DWY6) CONCRETE DRIVEWAY 6-INCH | (4B) CURB RAMP TYPE 4B |
| (ASD) ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES | (4B1) CURB RAMP TYPE 4B1 |
| (CG18D) CONCRETE CURB & GUTTER 18-INCH TYPE D | (4B1M) CURB RAMP TYPE 4B1 - MODIFIED |
| (CG24D) CONCRETE CURB & GUTTER 24-INCH TYPE D | XXXXXX SAWING ASPHALT |
| (CG30D) CONCRETE CURB & GUTTER 30-INCH TYPE D | AAAAAA SAWING CONCRETE |
| (PED) CONCRETE CURB PEDESTRIAN SPECIAL | XXX.XX PAVEMENT OR CURB FLANGE ELEVATION |
| (CCA) CONCRETE CURB TYPE A | XXX.XX(E) EXISTING ELEVATION |
| (CCD) CONCRETE CURB TYPE D | (HP) HIGH POINT |
| (SOD) TOPSOIL, FERTILIZER TYPE B, SOD LAWN | (LP) LOW POINT |
| (SEED) SEEDING MIXTURE NO. 40, FERTILIZER TYPE B, SALVAGED TOPSOIL, AND MULCH OR EROSION MAT | |

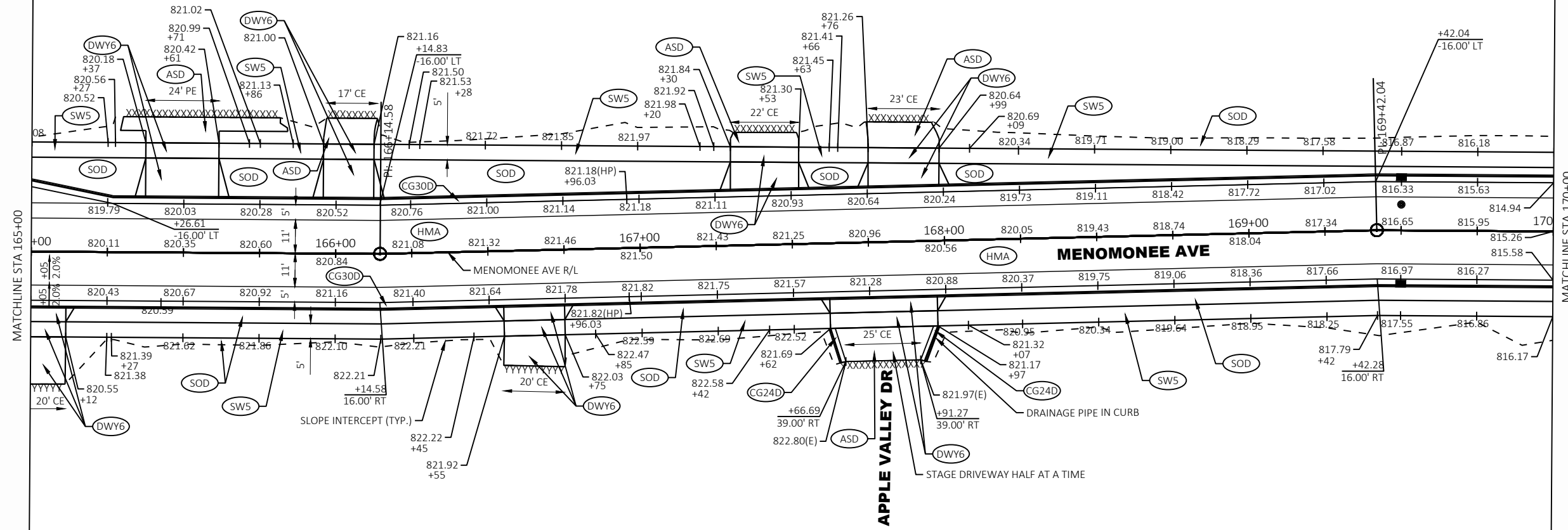


LEGEND

- | | |
|--|--|
| (HMA) 5" HMA PAVEMENT | (2) CURB RAMP TYPE 2 |
| (SW5) CONCRETE SIDEWALK 5-INCH | (2M) CURB RAMP TYPE 2 - MODIFIED |
| (DWY6) CONCRETE DRIVEWAY 6-INCH | (4B) CURB RAMP TYPE 4B |
| (ASD) ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES | (4B1) CURB RAMP TYPE 4B1 |
| (CG18D) CONCRETE CURB & GUTTER 18-INCH TYPE D | (4B1M) CURB RAMP TYPE 4B1 - MODIFIED |
| (CG24D) CONCRETE CURB & GUTTER 24-INCH TYPE D | XXXXXX SAWING ASPHALT |
| (CG30D) CONCRETE CURB & GUTTER 30-INCH TYPE D | AAAAAA SAWING CONCRETE |
| (PED) CONCRETE CURB PEDESTRIAN SPECIAL | XXX.XX PAVEMENT OR CURB FLANGE ELEVATION |
| (CCA) CONCRETE CURB TYPE A | XXX.XX(E) EXISTING ELEVATION |
| (CCD) CONCRETE CURB TYPE D | (HP) HIGH POINT |
| (SOD) TOPSOIL, FERTILIZER TYPE B, SOD LAWN | (LP) LOW POINT |
| (SEED) SEEDING MIXTURE NO. 40, FERTILIZER TYPE B, SALVAGED TOPSOIL, AND MULCH OR EROSION MAT | |

LEGEND

- (HMA) 5" HMA PAVEMENT
- (SW5) CONCRETE SIDEWALK 5-INCH
- (DWY6) CONCRETE DRIVEWAY 6-INCH
- (ASD) ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES
- (CG18D) CONCRETE CURB & GUTTER 18-INCH TYPE D
- (CG24D) CONCRETE CURB & GUTTER 24-INCH TYPE D
- (CG30D) CONCRETE CURB & GUTTER 30-INCH TYPE D
- (PED) CONCRETE CURB PEDESTRIAN SPECIAL
- (CCA) CONCRETE CURB TYPE A
- (CCD) CONCRETE CURB TYPE D
- (SOD) TOPSOIL, FERTILIZER TYPE B, SOD LAWN
- (SEED) SEEDING MIXTURE NO. 40, FERTILIZER TYPE B, SALVAGED TOPSOIL, AND MULCH OR EROSION MAT
- (2) CURB RAMP TYPE 2
- (2M) CURB RAMP TYPE 2 - MODIFIED
- (4B) CURB RAMP TYPE 4B
- (4B1) CURB RAMP TYPE 4B1
- (4B1M) CURB RAMP TYPE 4B1 - MODIFIED
- XXXXXX SAWING ASPHALT
- AAAAAA SAWING CONCRETE
- XXX.XX PAVEMENT OR CURB FLANGE ELEVATION
- XXX.XX(E) EXISTING ELEVATION
- (HP) HIGH POINT
- (LP) LOW POINT

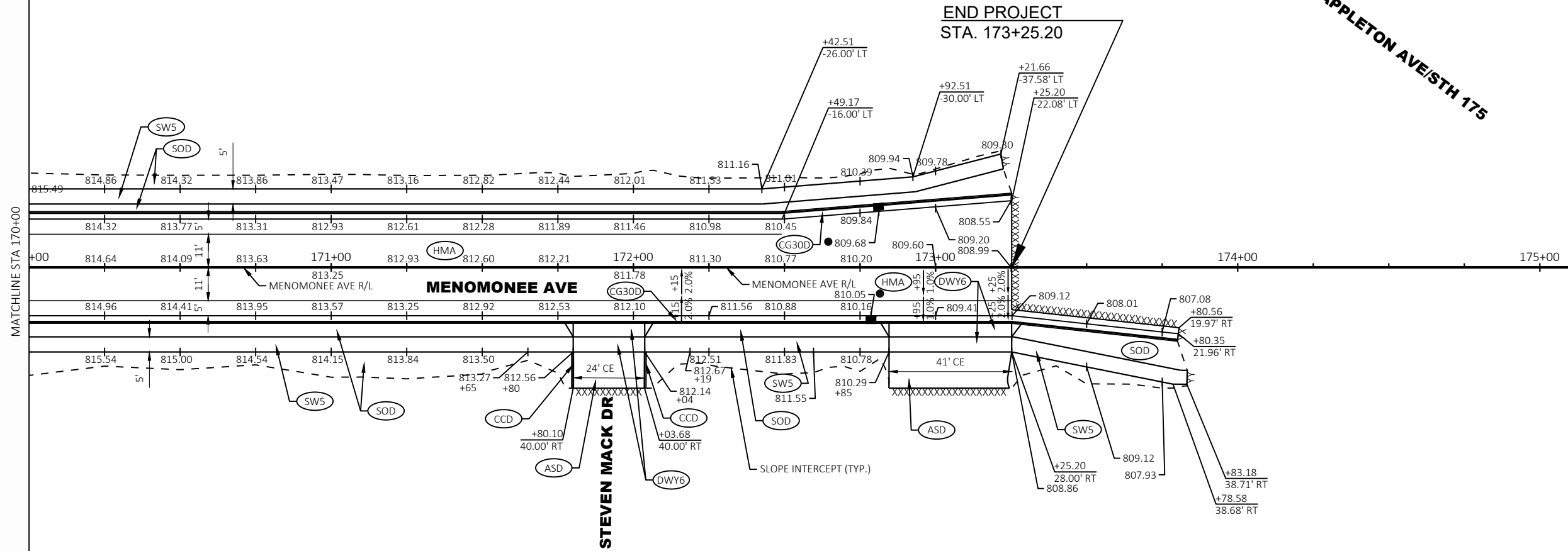


LEGEND

- (HMA) 5" HMA PAVEMENT
- (SW5) CONCRETE SIDEWALK 5-INCH
- (DWY6) CONCRETE DRIVEWAY 6-INCH
- (ASD) ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES
- (CG18D) CONCRETE CURB & GUTTER 18-INCH TYPE D
- (CG24D) CONCRETE CURB & GUTTER 24-INCH TYPE D
- (CG30D) CONCRETE CURB & GUTTER 30-INCH TYPE D
- (PED) CONCRETE CURB PEDESTRIAN SPECIAL
- (CCA) CONCRETE CURB TYPE A
- (CCD) CONCRETE CURB TYPE D
- (SOD) TOPSOIL, FERTILIZER TYPE B, SOD LAWN
- (SEED) SEEDING MIXTURE NO. 40, FERTILIZER TYPE B, SALVAGED TOPSOIL, AND MULCH OR EROSION MAT
- (2) CURB RAMP TYPE 2
- (2M) CURB RAMP TYPE 2 - MODIFIED
- (4B) CURB RAMP TYPE 4B
- (4B1) CURB RAMP TYPE 4B1
- (4B1M) CURB RAMP TYPE 4B1 - MODIFIED
- XXXXXX SAWING ASPHALT
- AAAAAA SAWING CONCRETE
- XXX.XX PAVEMENT OR CURB FLANGE ELEVATION
- XXX.XX(E) EXISTING ELEVATION
- (HP) HIGH POINT
- (LP) LOW POINT



APPLETON AVE/STH 175

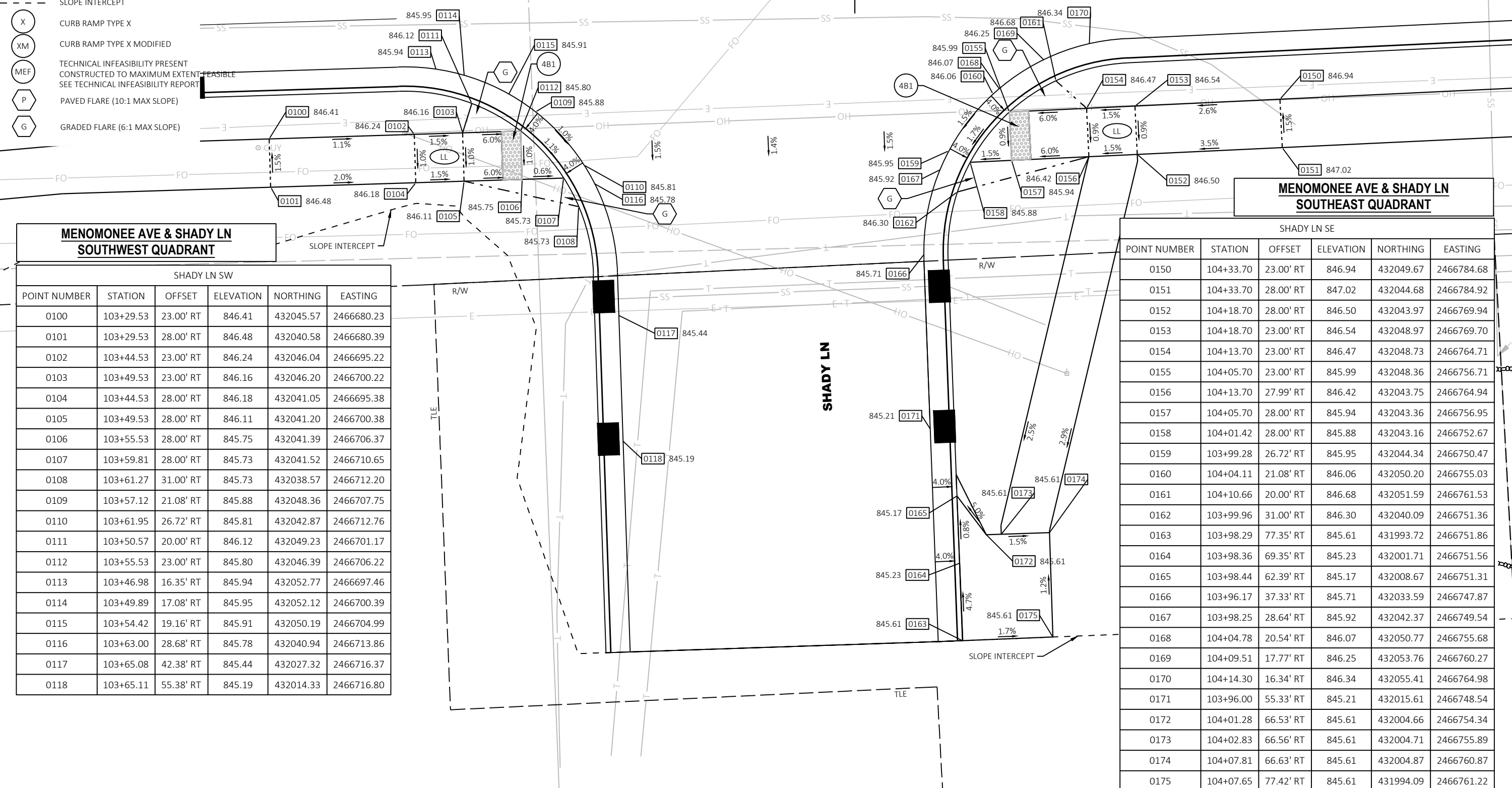


LEGEND

- XXX.XX FINISHED ELEVATION
- HP / LP HIGH POINT / LOW POINT
- LL LEVEL LANDING
- 100 POINT NUMBER
- DETECTABLE WARNING FIELD
- SLOPE INTERCEPT
- X CURB RAMP TYPE X
- XM CURB RAMP TYPE X MODIFIED
- MEF TECHNICAL INFEASIBILITY PRESENT CONSTRUCTED TO MAXIMUM EXTENT FEASIBLE SEE TECHNICAL INFEASIBILITY REPORT
- P PAVED FLARE (10:1 MAX SLOPE)
- G GRADED FLARE (6:1 MAX SLOPE)

NOTES

1. CONTRACTOR TO FIELD VERIFY ELEVATIONS, GRADES, SLOPES, LENGTHS, AND MATCH POINTS PRIOR TO CURB RAMP CONSTRUCTION.
2. THE ENGINEER MAY ADJUST ELEVATIONS TO FIT FIELD CONDITIONS WITHIN THE CONDITIONS OF THE STANDARD DETAIL DRAWINGS.
3. DASHED LINES SHOWN FOR PROPOSED SIDEWALK ARE FOR INFORMATION ONLY AND DO NOT INDICATE EXACT JOINT LOCATIONS.
4. THE CROSS SLOPE OF THE GUTTER SHALL BE 4.0% UNLESS OTHERWISE SHOWN.
5. THE MAXIMUM GRADE BREAK BETWEEN THE GUTTER PAN AND CURB RAMP SHALL BE 11%.
6. SIDEWALK AND CURB RAMP CROSS SLOPE SHALL NOT EXCEED 2%.
7. SIDEWALK AND CURB RAMP RUNNING SLOPE SHALL NOT EXCEED 8.33% (12H:1V).
8. PAVED FLARES SHALL BE 3' LONG MEASURED ALONG THE CURB, UNLESS OTHERWISE NOTED.



**MENOMONEE AVE & SHADY LN
SOUTHWEST QUADRANT**

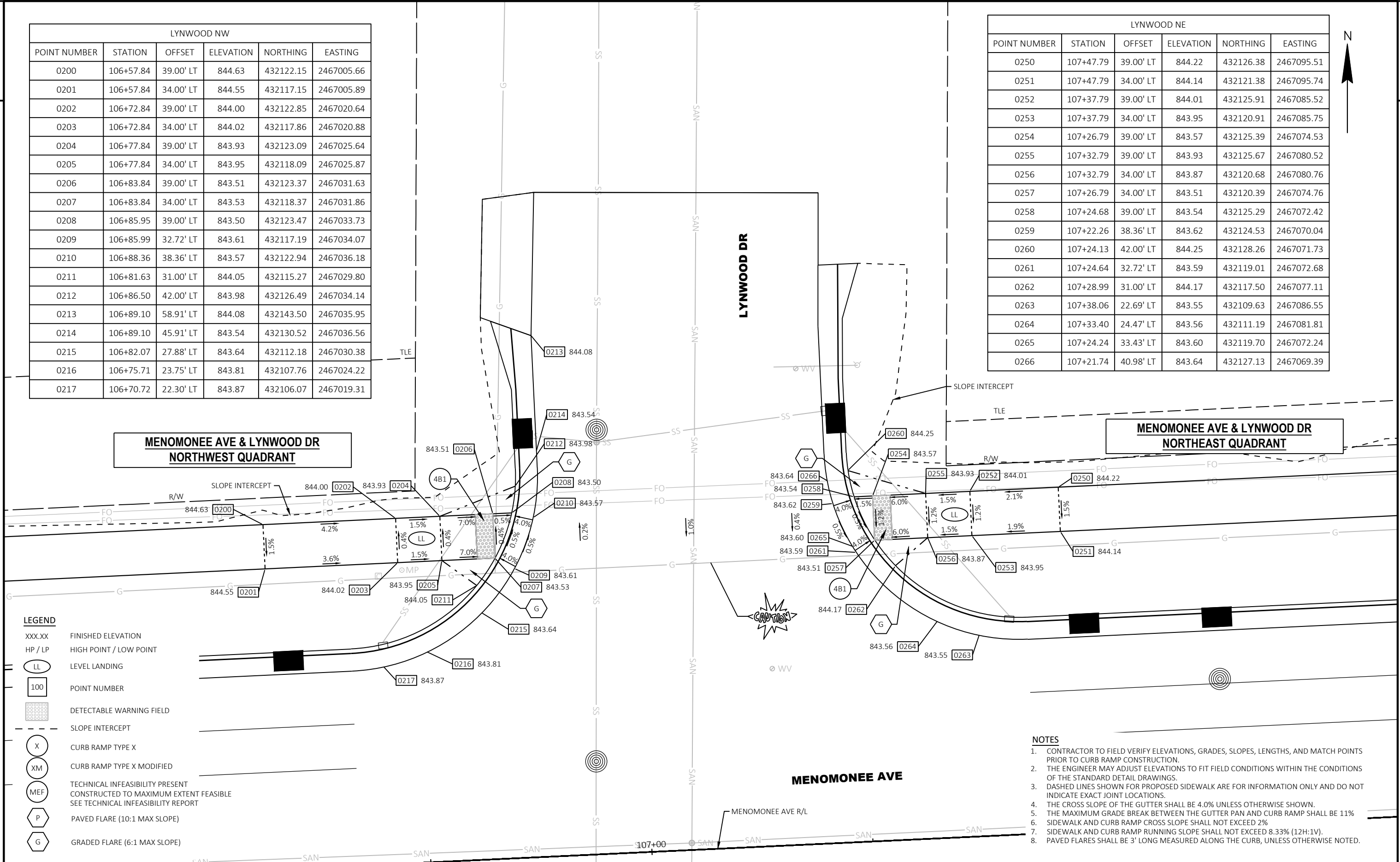
SHADY LN SW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
0100	103+29.53	23.00' RT	846.41	432045.57	2466680.23
0101	103+29.53	28.00' RT	846.48	432040.58	2466680.39
0102	103+44.53	23.00' RT	846.24	432046.04	2466695.22
0103	103+49.53	23.00' RT	846.16	432046.20	2466700.22
0104	103+44.53	28.00' RT	846.18	432041.05	2466695.38
0105	103+49.53	28.00' RT	846.11	432041.20	2466700.38
0106	103+55.53	28.00' RT	845.75	432041.39	2466706.37
0107	103+59.81	28.00' RT	845.73	432041.52	2466710.65
0108	103+61.27	31.00' RT	845.73	432038.57	2466712.20
0109	103+57.12	21.08' RT	845.88	432048.36	2466707.75
0110	103+61.95	26.72' RT	845.81	432042.87	2466712.76
0111	103+50.57	20.00' RT	846.12	432049.23	2466701.17
0112	103+55.53	23.00' RT	845.80	432046.39	2466706.22
0113	103+46.98	16.35' RT	845.94	432052.77	2466697.46
0114	103+49.89	17.08' RT	845.95	432052.12	2466700.39
0115	103+54.42	19.16' RT	845.91	432050.19	2466704.99
0116	103+63.00	28.68' RT	845.78	432040.94	2466713.86
0117	103+65.08	42.38' RT	845.44	432027.32	2466716.37
0118	103+65.11	55.38' RT	845.19	432014.33	2466716.80

**MENOMONEE AVE & SHADY LN
SOUTHEAST QUADRANT**

SHADY LN SE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
0150	104+33.70	23.00' RT	846.94	432049.67	2466784.68
0151	104+33.70	28.00' RT	847.02	432044.68	2466784.92
0152	104+18.70	28.00' RT	846.50	432043.97	2466769.94
0153	104+18.70	23.00' RT	846.54	432048.97	2466769.70
0154	104+13.70	23.00' RT	846.47	432048.73	2466764.71
0155	104+05.70	23.00' RT	845.99	432048.36	2466756.71
0156	104+13.70	27.99' RT	846.42	432043.75	2466764.94
0157	104+05.70	28.00' RT	845.94	432043.36	2466756.95
0158	104+01.42	28.00' RT	845.88	432043.16	2466752.67
0159	103+99.28	26.72' RT	845.95	432044.34	2466750.47
0160	104+04.11	21.08' RT	846.06	432050.20	2466755.03
0161	104+10.66	20.00' RT	846.68	432051.59	2466761.53
0162	103+99.96	31.00' RT	846.30	432040.09	2466751.36
0163	103+98.29	77.35' RT	845.61	431993.72	2466751.86
0164	103+98.36	69.35' RT	845.23	432001.71	2466751.56
0165	103+98.44	62.39' RT	845.17	432008.67	2466751.31
0166	103+96.17	37.33' RT	845.71	432033.59	2466747.87
0167	103+98.25	28.64' RT	845.92	432042.37	2466749.54
0168	104+04.78	20.54' RT	846.07	432050.77	2466755.68
0169	104+09.51	17.77' RT	846.25	432053.76	2466760.27
0170	104+14.30	16.34' RT	846.34	432055.41	2466764.98
0171	103+96.00	55.33' RT	845.21	432015.61	2466748.54
0172	104+01.28	66.53' RT	845.61	432004.66	2466754.34
0173	104+02.83	66.56' RT	845.61	432004.71	2466755.89
0174	104+07.81	66.63' RT	845.61	432004.87	2466760.87
0175	104+07.65	77.42' RT	845.61	431994.09	2466761.22

LYNWOOD NW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
0200	106+57.84	39.00' LT	844.63	432122.15	2467005.66
0201	106+57.84	34.00' LT	844.55	432117.15	2467005.89
0202	106+72.84	39.00' LT	844.00	432122.85	2467020.64
0203	106+72.84	34.00' LT	844.02	432117.86	2467020.88
0204	106+77.84	39.00' LT	843.93	432123.09	2467025.64
0205	106+77.84	34.00' LT	843.95	432118.09	2467025.87
0206	106+83.84	39.00' LT	843.51	432123.37	2467031.63
0207	106+83.84	34.00' LT	843.53	432118.37	2467031.86
0208	106+85.95	39.00' LT	843.50	432123.47	2467033.73
0209	106+85.99	32.72' LT	843.61	432117.19	2467034.07
0210	106+88.36	38.36' LT	843.57	432122.94	2467036.18
0211	106+81.63	31.00' LT	844.05	432115.27	2467029.80
0212	106+86.50	42.00' LT	843.98	432126.49	2467034.14
0213	106+89.10	58.91' LT	844.08	432143.50	2467035.95
0214	106+89.10	45.91' LT	843.54	432130.52	2467036.56
0215	106+82.07	27.88' LT	843.64	432112.18	2467030.38
0216	106+75.71	23.75' LT	843.81	432107.76	2467024.22
0217	106+70.72	22.30' LT	843.87	432106.07	2467019.31

LYNWOOD NE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
0250	107+47.79	39.00' LT	844.22	432126.38	2467095.51
0251	107+47.79	34.00' LT	844.14	432121.38	2467095.74
0252	107+37.79	39.00' LT	844.01	432125.91	2467085.52
0253	107+37.79	34.00' LT	843.95	432120.91	2467085.75
0254	107+26.79	39.00' LT	843.57	432125.39	2467074.53
0255	107+32.79	39.00' LT	843.93	432125.67	2467080.52
0256	107+32.79	34.00' LT	843.87	432120.68	2467080.76
0257	107+26.79	34.00' LT	843.51	432120.39	2467074.76
0258	107+24.68	39.00' LT	843.54	432125.29	2467072.42
0259	107+22.26	38.36' LT	843.62	432124.53	2467070.04
0260	107+24.13	42.00' LT	844.25	432128.26	2467071.73
0261	107+24.64	32.72' LT	843.59	432119.01	2467072.68
0262	107+28.99	31.00' LT	844.17	432117.50	2467077.11
0263	107+38.06	22.69' LT	843.55	432109.63	2467086.55
0264	107+33.40	24.47' LT	843.56	432111.19	2467081.81
0265	107+24.24	33.43' LT	843.60	432119.70	2467072.24
0266	107+21.74	40.98' LT	843.64	432127.13	2467069.39



**MENOMONEE AVE & LYNWOOD DR
NORTHWEST QUADRANT**

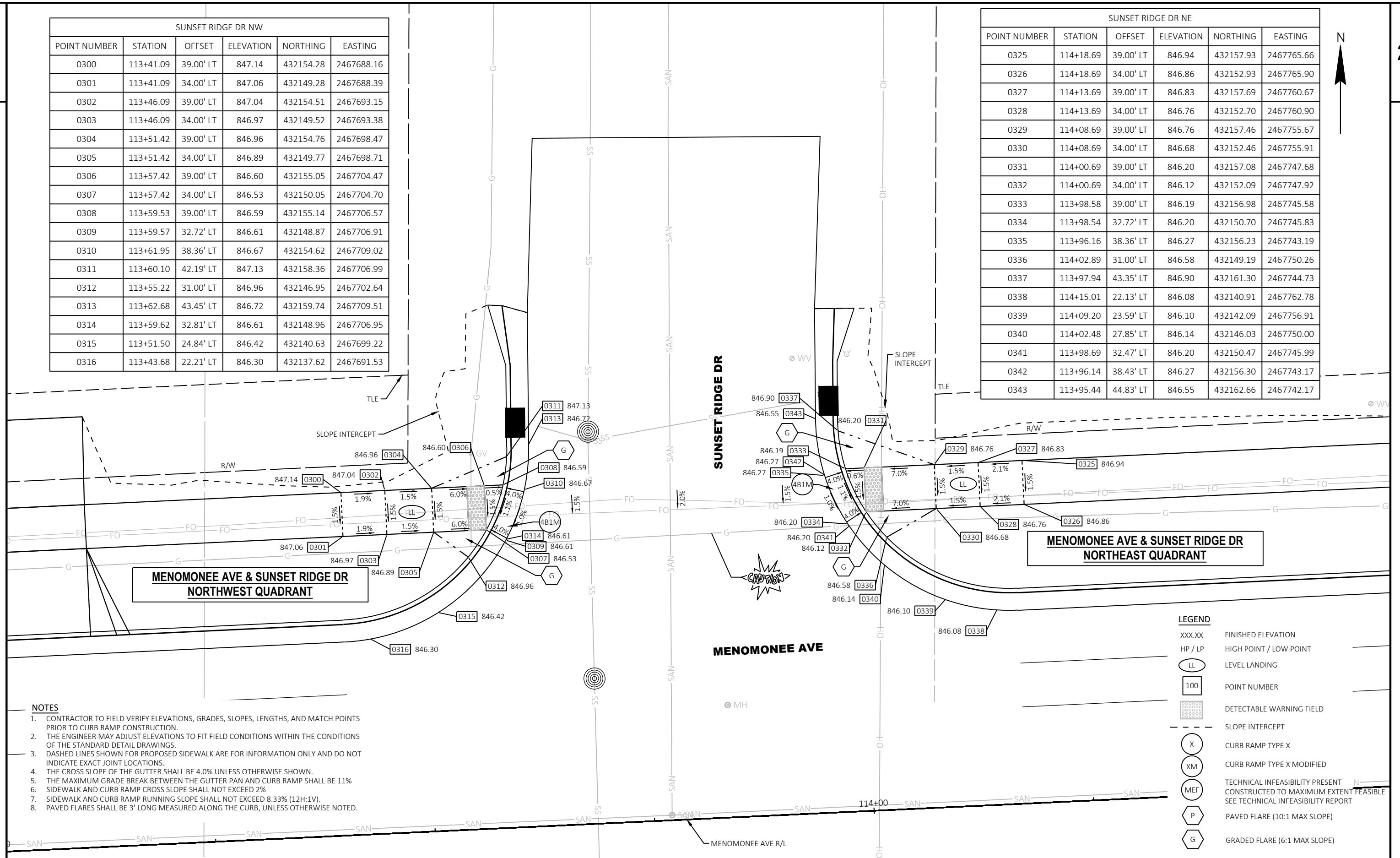
**MENOMONEE AVE & LYNWOOD DR
NORTHEAST QUADRANT**

- LEGEND**
- XXX.XX FINISHED ELEVATION
 - HP / LP HIGH POINT / LOW POINT
 - LL LEVEL LANDING
 - 100 POINT NUMBER
 - [Pattern] DETECTABLE WARNING FIELD
 - - - SLOPE INTERCEPT
 - X CURB RAMP TYPE X
 - XM CURB RAMP TYPE X MODIFIED
 - MEF TECHNICAL INFEASIBILITY PRESENT
CONSTRUCTED TO MAXIMUM EXTENT FEASIBLE
SEE TECHNICAL INFEASIBILITY REPORT
 - P PAVED FLARE (10:1 MAX SLOPE)
 - G GRADED FLARE (6:1 MAX SLOPE)

- NOTES**
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 7. SIDEWALK AND CURB RAMP RUNNING SLOPE SHALL NOT EXCEED 8.33% (12H:1V).
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SUNSET RIDGE DR NW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
0300	113+41.09	39.00' LT	847.14	432154.28	2467688.16
0301	113+41.09	34.00' LT	847.06	432149.28	2467688.39
0302	113+46.09	39.00' LT	847.04	432154.51	2467693.15
0303	113+46.09	34.00' LT	846.97	432149.52	2467693.38
0304	113+51.42	39.00' LT	846.96	432154.76	2467698.47
0305	113+51.42	34.00' LT	846.89	432149.77	2467698.71
0306	113+57.42	39.00' LT	846.60	432155.05	2467704.47
0307	113+57.42	34.00' LT	846.53	432150.05	2467704.70
0308	113+59.53	39.00' LT	846.59	432155.14	2467706.57
0309	113+59.57	32.72' LT	846.61	432148.87	2467706.91
0310	113+61.95	38.36' LT	846.67	432154.62	2467709.02
0311	113+60.10	42.19' LT	847.13	432158.36	2467706.99
0312	113+55.22	31.00' LT	846.96	432146.95	2467702.64
0313	113+62.68	43.45' LT	846.72	432159.74	2467709.51
0314	113+59.62	32.81' LT	846.61	432148.96	2467706.95
0315	113+51.50	24.84' LT	846.42	432140.63	2467699.22
0316	113+43.68	22.21' LT	846.30	432137.62	2467691.53

SUNSET RIDGE DR NE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
0325	114+18.69	39.00' LT	846.94	432157.93	2467765.66
0326	114+18.69	34.00' LT	846.86	432152.93	2467765.90
0327	114+13.69	39.00' LT	846.83	432157.69	2467760.67
0328	114+13.69	34.00' LT	846.76	432152.70	2467760.90
0329	114+08.69	39.00' LT	846.76	432157.46	2467755.67
0330	114+08.69	34.00' LT	846.68	432152.46	2467755.91
0331	114+00.69	39.00' LT	846.20	432157.08	2467747.68
0332	114+00.69	34.00' LT	846.12	432152.09	2467747.92
0333	113+98.58	39.00' LT	846.19	432156.98	2467745.58
0334	113+98.54	32.72' LT	846.20	432150.70	2467745.83
0335	113+96.16	38.36' LT	846.27	432156.23	2467743.19
0336	114+02.89	31.00' LT	846.58	432149.19	2467750.26
0337	113+97.94	43.35' LT	846.90	432161.30	2467744.73
0338	114+15.01	22.13' LT	846.08	432140.91	2467762.78
0339	114+09.20	23.59' LT	846.10	432142.09	2467756.91
0340	114+02.48	27.85' LT	846.14	432146.03	2467750.00
0341	113+98.69	32.47' LT	846.20	432150.47	2467745.99
0342	113+96.14	38.43' LT	846.27	432156.30	2467743.17
0343	113+95.44	44.83' LT	846.55	432162.66	2467742.17



**MENOMONEE AVE & SUNSET RIDGE DR
NORTHWEST QUADRANT**

**MENOMONEE AVE & SUNSET RIDGE DR
NORTHEAST QUADRANT**

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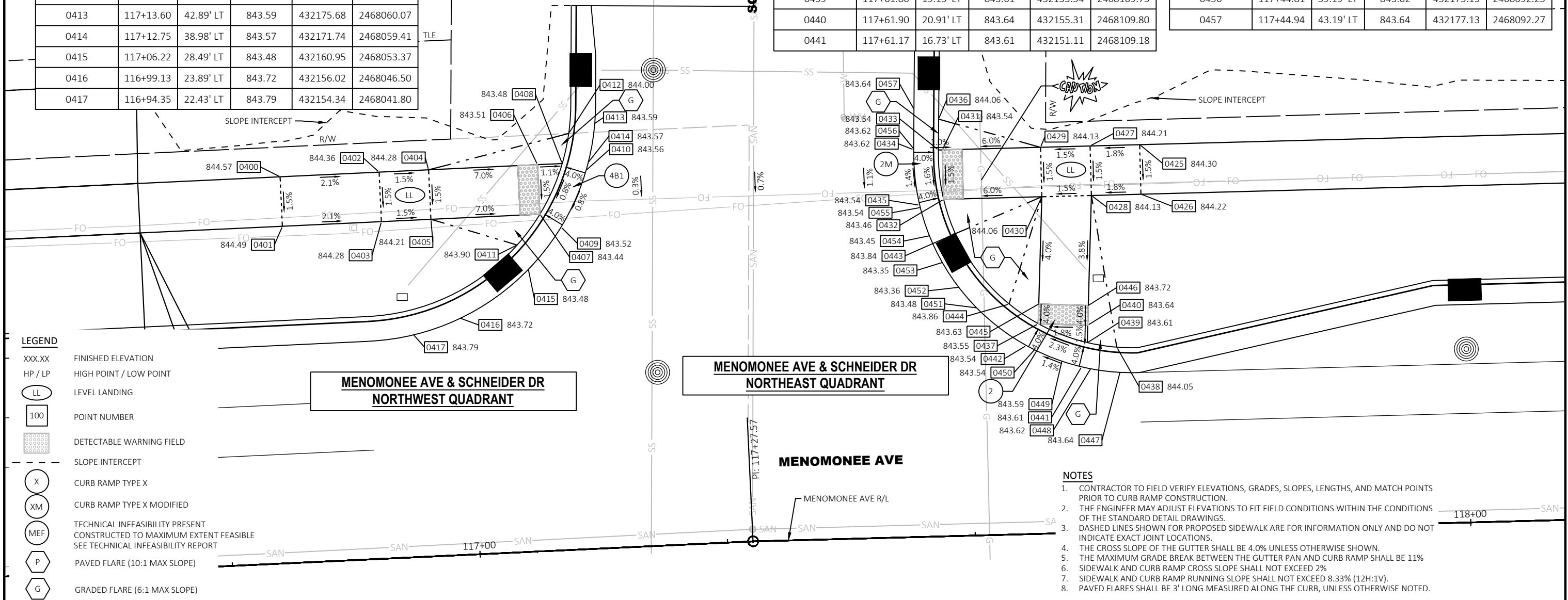
LEGEND

XXX.XX	FINISHED ELEVATION
HP / LP	HIGH POINT / LOW POINT
LL	LEVEL LANDING
100	POINT NUMBER
[Pattern]	DETECTABLE WARNING FIELD
---	SLOPE INTERCEPT
X	CURB RAMP TYPE X
XM	CURB RAMP TYPE X MODIFIED
MEF	TECHNICAL INFESIBILITY PRESENT CONSTRUCTED TO MAXIMUM EXTENT FEASIBLE SEE TECHNICAL INFESIBILITY REPORT
P	PAVED FLARE (10:1 MAX SLOPE)
G	GRADED FLARE (6:1 MAX SLOPE)

SCHNEIDER DR NW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
0400	116+81.64	39.00' LT	844.57	432170.29	2468028.33
0401	116+81.64	34.00' LT	844.49	432165.30	2468028.56
0402	116+91.64	39.00' LT	844.36	432170.76	2468038.32
0403	116+91.64	34.00' LT	844.28	432165.77	2468038.55
0404	116+96.64	39.00' LT	844.28	432171.00	2468043.31
0405	116+96.64	34.00' LT	844.21	432166.00	2468043.55
0406	117+07.64	39.00' LT	843.51	432171.52	2468054.30
0407	117+07.64	34.00' LT	843.44	432166.52	2468054.53
0408	117+10.13	39.00' LT	843.48	432171.63	2468056.79
0409	117+09.72	32.60' LT	843.52	432165.23	2468056.67
0410	117+12.50	38.19' LT	843.56	432170.93	2468059.18
0411	117+05.21	31.00' LT	843.90	432163.41	2468052.24
0412	117+10.93	42.00' LT	844.00	432174.67	2468057.44
0413	117+13.60	42.89' LT	843.59	432175.68	2468060.07
0414	117+12.75	38.98' LT	843.57	432171.74	2468059.41
0415	117+06.22	28.49' LT	843.48	432160.95	2468053.37
0416	116+99.13	23.89' LT	843.72	432156.02	2468046.50
0417	116+94.35	22.43' LT	843.79	432154.34	2468041.80

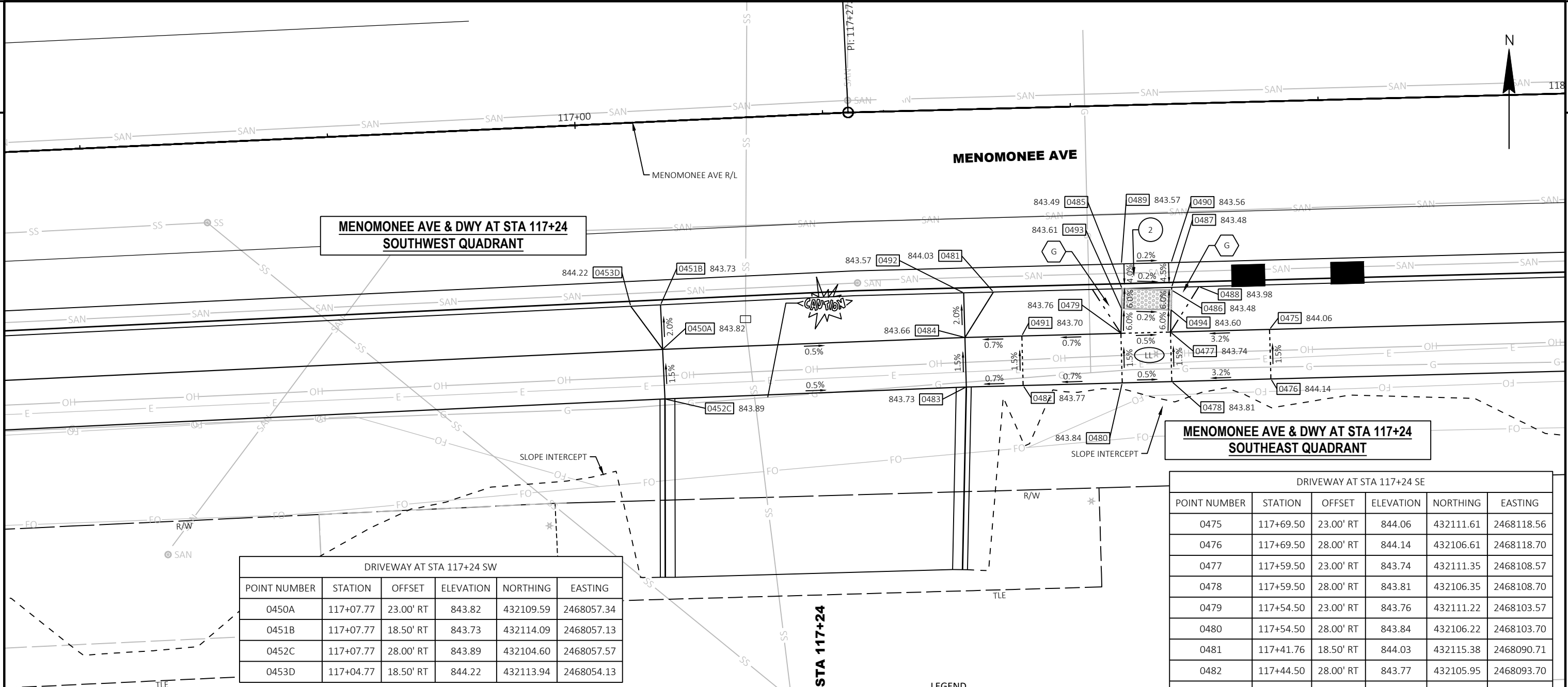
SCHNEIDER DR NE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
0425	117+67.62	39.00' LT	844.30	432173.54	2468115.04
0426	117+67.62	34.00' LT	844.22	432168.54	2468115.17
0427	117+62.62	39.00' LT	844.21	432173.41	2468110.04
0428	117+62.62	34.00' LT	844.13	432168.41	2468110.17
0429	117+57.62	39.00' LT	844.13	432173.28	2468105.04
0430	117+57.62	34.00' LT	844.06	432168.28	2468105.17
0431	117+47.69	39.00' LT	843.54	432173.01	2468095.13
0432	117+47.69	34.00' LT	843.46	432168.02	2468095.26
0433	117+47.30	39.00' LT	843.54	432173.00	2468094.73
0434	117+44.80	39.09' LT	843.62	432173.02	2468092.23
0435	117+45.25	33.49' LT	843.54	432167.44	2468092.82
0436	117+47.41	42.11' LT	844.06	432176.11	2468094.76
0437	117+56.91	21.18' LT	843.55	432155.45	2468104.81
0438	117+64.78	18.60' LT	844.05	432153.08	2468112.74
0439	117+61.80	19.15' LT	843.61	432153.54	2468109.75
0440	117+61.90	20.91' LT	843.64	432155.31	2468109.80
0441	117+61.17	16.73' LT	843.61	432151.11	2468109.18

SCHNEIDER DR NE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
0442	117+55.64	19.03' LT	843.54	432153.26	2468103.60
0443	117+48.78	30.50' LT	843.84	432164.55	2468096.43
0444	117+54.02	23.26' LT	843.86	432157.44	2468101.86
0445	117+57.02	23.18' LT	843.63	432157.45	2468104.86
0446	117+62.01	22.91' LT	843.72	432157.31	2468109.86
0447	117+65.03	16.07' LT	843.64	432150.55	2468113.06
0448	117+62.06	16.51' LT	843.62	432150.91	2468110.08
0449	117+59.19	17.35' LT	843.59	432151.68	2468107.18
0450	117+56.45	18.58' LT	843.54	432152.83	2468104.42
0451	117+50.72	22.96' LT	843.48	432157.06	2468098.57
0452	117+48.97	25.08' LT	843.36	432159.14	2468096.77
0453	117+47.38	27.62' LT	843.35	432161.63	2468095.11
0454	117+46.15	30.36' LT	843.45	432164.33	2468093.81
0455	117+45.30	33.23' LT	843.54	432167.19	2468092.89
0456	117+44.81	39.19' LT	843.62	432173.13	2468092.23
0457	117+44.94	43.19' LT	843.64	432177.13	2468092.27



- LEGEND**
- XXX.XX FINISHED ELEVATION
 - HP / LP HIGH POINT / LOW POINT
 - LL LEVEL LANDING
 - 100 POINT NUMBER
 - DETECTABLE WARNING FIELD
 - SLOPE INTERCEPT
 - X CURB RAMP TYPE X
 - XM CURB RAMP TYPE X MODIFIED
 - MEF TECHNICAL INFEASIBILITY PRESENT
CONSTRUCTED TO MAXIMUM EXTENT FEASIBLE
SEE TECHNICAL INFEASIBILITY REPORT
 - P PAVED FLARE (10:1 MAX SLOPE)
 - G GRADED FLARE (6:1 MAX SLOPE)

- NOTES**
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DRIVEWAY AT STA 117+24 SW

POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
0450A	117+07.77	23.00' RT	843.82	432109.59	2468057.34
0451B	117+07.77	18.50' RT	843.73	432114.09	2468057.13
0452C	117+07.77	28.00' RT	843.89	432104.60	2468057.57
0453D	117+04.77	18.50' RT	844.22	432113.94	2468054.13

DRIVEWAY AT STA 117+24 SE

POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
0475	117+69.50	23.00' RT	844.06	432111.61	2468118.56
0476	117+69.50	28.00' RT	844.14	432106.61	2468118.70
0477	117+59.50	23.00' RT	843.74	432111.35	2468108.57
0478	117+59.50	28.00' RT	843.81	432106.35	2468108.70
0479	117+54.50	23.00' RT	843.76	432111.22	2468103.57
0480	117+54.50	28.00' RT	843.84	432106.22	2468103.70
0481	117+41.76	18.50' RT	844.03	432115.38	2468090.71
0482	117+44.50	28.00' RT	843.77	432105.95	2468093.70
0483	117+38.76	28.00' RT	843.73	432105.80	2468087.97
0484	117+38.76	23.00' RT	843.66	432110.80	2468087.83
0485	117+54.74	18.50' RT	843.49	432115.72	2468103.69
0486	117+59.73	18.77' RT	843.48	432115.58	2468108.69
0487	117+59.75	18.50' RT	843.48	432115.85	2468108.69
0488	117+62.50	18.50' RT	843.98	432115.93	2468111.45
0489	117+54.74	16.00' RT	843.57	432118.22	2468103.62
0490	117+59.75	16.00' RT	843.56	432118.35	2468108.63
0491	117+44.50	23.00' RT	843.70	432110.95	2468093.57
0492	117+38.76	18.50' RT	843.57	432115.30	2468087.72
0493	117+54.63	20.50' RT	843.61	432113.72	2468103.63
0494	117+59.63	20.77' RT	843.60	432113.58	2468108.63

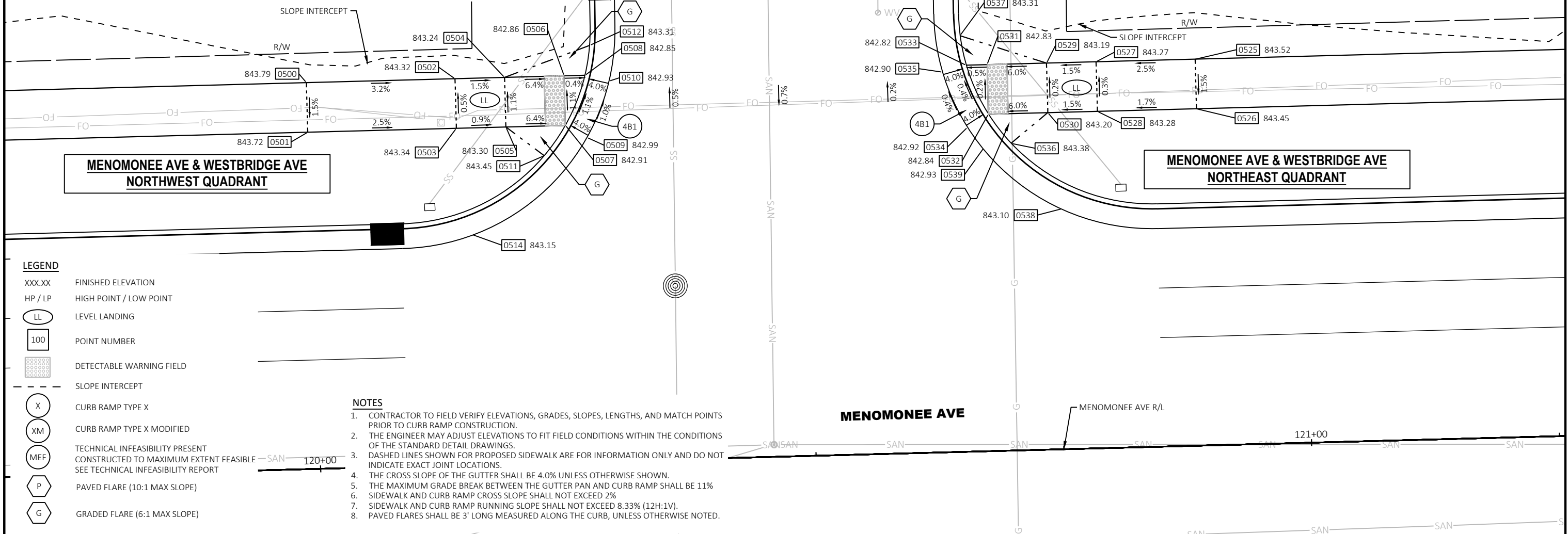
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LEGEND

- XXX.XX FINISHED ELEVATION
- HP / LP HIGH POINT / LOW POINT
- LL LEVEL LANDING
- 100 POINT NUMBER
- [Pattern] DETECTABLE WARNING FIELD
- - - SLOPE INTERCEPT
- (X) CURB RAMP TYPE X
- (XM) CURB RAMP TYPE X MODIFIED
- (MEF) TECHNICAL INFEASIBILITY PRESENT CONSTRUCTED TO MAXIMUM EXTENT FEASIBLE SEE TECHNICAL INFEASIBILITY REPORT
- (P) PAVED FLARE (10:1 MAX SLOPE)
- (G) GRADED FLARE (6:1 MAX SLOPE)

WESTBRIDGE AVE NW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
0500	119+99.59	39.00' LT	843.79	432179.66	2468346.93
0501	119+99.59	34.00' LT	843.72	432174.67	2468347.07
0502	120+14.59	39.00' LT	843.32	432180.06	2468361.93
0503	120+14.59	34.00' LT	843.34	432175.06	2468362.06
0504	120+19.59	39.00' LT	843.24	432180.19	2468366.93
0505	120+19.59	34.00' LT	843.30	432175.19	2468367.06
0506	120+25.59	39.00' LT	842.86	432180.35	2468372.92
0507	120+25.59	34.00' LT	842.91	432175.35	2468373.06
0508	120+27.70	39.00' LT	842.85	432180.41	2468375.03
0509	120+27.74	32.72' LT	842.99	432174.13	2468375.24
0510	120+30.11	38.36' LT	842.93	432179.83	2468377.46
0511	120+23.38	31.00' LT	843.45	432172.30	2468370.93
0512	120+28.25	42.00' LT	843.31	432183.42	2468375.50
0513	120+30.88	47.28' LT	842.84	432188.77	2468377.99
0514	120+16.00	23.19' LT	843.15	432164.30	2468363.75

WESTBRIDGE AVE NE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
0525	120+89.28	39.00' LT	843.52	432182.03	2468436.59
0526	120+89.28	34.00' LT	843.45	432177.03	2468436.72
0527	120+79.28	39.00' LT	843.27	432181.77	2468426.60
0528	120+79.28	34.00' LT	843.28	432176.77	2468426.73
0529	120+74.28	39.00' LT	843.19	432181.64	2468421.60
0530	120+74.28	34.00' LT	843.20	432176.64	2468421.73
0531	120+68.28	39.00' LT	842.83	432181.48	2468415.60
0532	120+68.28	34.00' LT	842.84	432176.48	2468415.73
0533	120+66.17	39.00' LT	842.82	432181.42	2468413.49
0534	120+66.13	32.72' LT	842.92	432175.14	2468413.62
0535	120+63.76	38.36' LT	842.90	432180.72	2468411.09
0536	120+70.49	31.00' LT	843.38	432173.54	2468418.02
0537	120+65.62	42.00' LT	843.31	432184.41	2468412.86
0538	120+75.34	24.24' LT	843.10	432166.91	2468423.05
0539	120+67.65	30.50' LT	842.93	432172.96	2468415.20
0540	120+63.04	46.25' LT	842.87	432188.59	2468410.17
0541	120+63.08	49.51' LT	842.86	432191.85	2468410.12



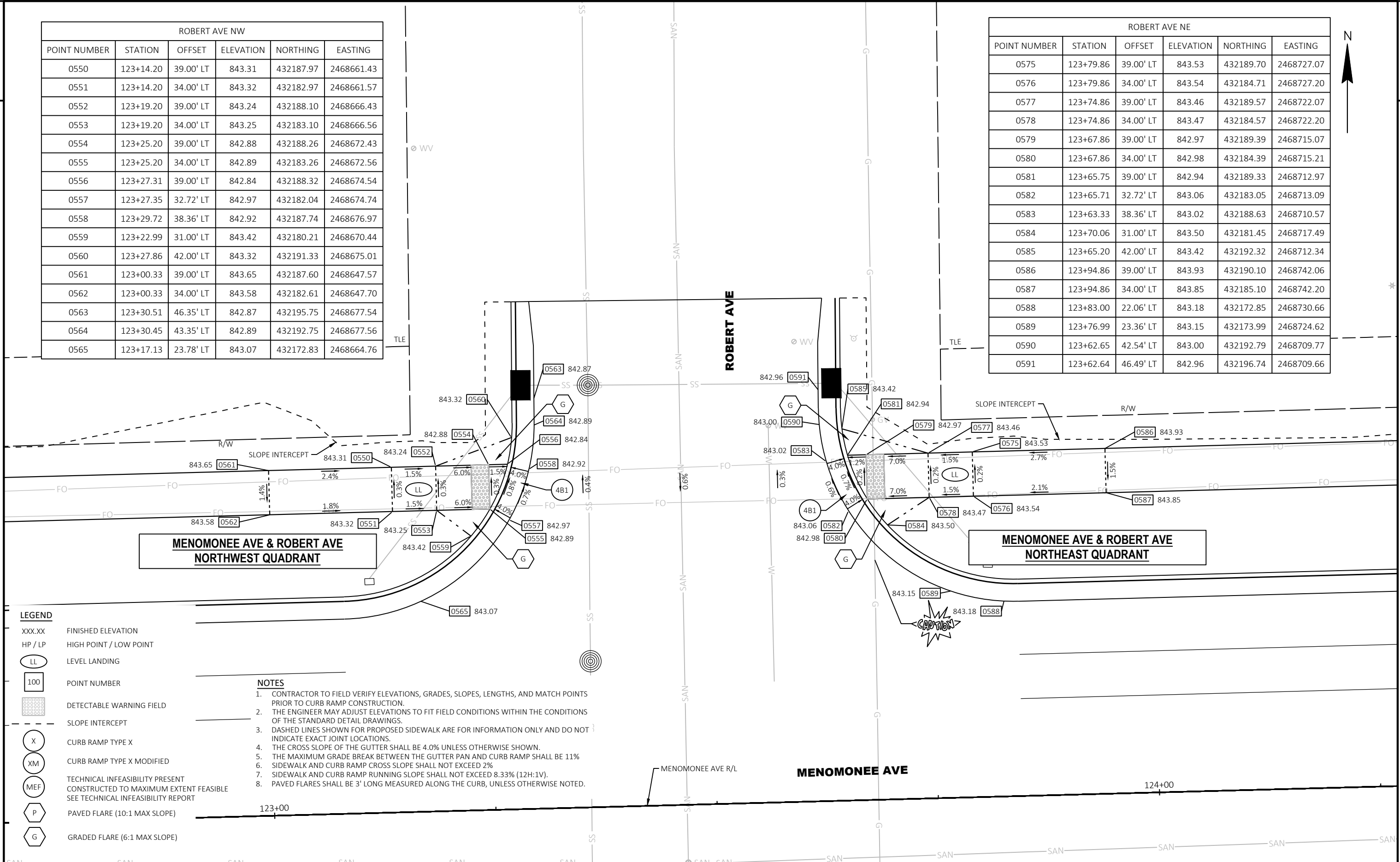
LEGEND

- XXX.XX FINISHED ELEVATION
- HP / LP HIGH POINT / LOW POINT
- LL LEVEL LANDING
- 100 POINT NUMBER
- [Pattern] DETECTABLE WARNING FIELD
- - - SLOPE INTERCEPT
- (X) CURB RAMP TYPE X
- (XM) CURB RAMP TYPE X MODIFIED
- (MEF) TECHNICAL INFEASIBILITY PRESENT CONSTRUCTED TO MAXIMUM EXTENT FEASIBLE - SEE TECHNICAL INFEASIBILITY REPORT
- (P) PAVED FLARE (10:1 MAX SLOPE)
- (G) GRADED FLARE (6:1 MAX SLOPE)

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ROBERT AVE NW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
0550	123+14.20	39.00' LT	843.31	432187.97	2468661.43
0551	123+14.20	34.00' LT	843.32	432182.97	2468661.57
0552	123+19.20	39.00' LT	843.24	432188.10	2468666.43
0553	123+19.20	34.00' LT	843.25	432183.10	2468666.56
0554	123+25.20	39.00' LT	842.88	432188.26	2468672.43
0555	123+25.20	34.00' LT	842.89	432183.26	2468672.56
0556	123+27.31	39.00' LT	842.84	432188.32	2468674.54
0557	123+27.35	32.72' LT	842.97	432182.04	2468674.74
0558	123+29.72	38.36' LT	842.92	432187.74	2468676.97
0559	123+22.99	31.00' LT	843.42	432180.21	2468670.44
0560	123+27.86	42.00' LT	843.32	432191.33	2468675.01
0561	123+00.33	39.00' LT	843.65	432187.60	2468647.57
0562	123+00.33	34.00' LT	843.58	432182.61	2468647.70
0563	123+30.51	46.35' LT	842.87	432195.75	2468677.54
0564	123+30.45	43.35' LT	842.89	432192.75	2468677.56
0565	123+17.13	23.78' LT	843.07	432172.83	2468664.76

ROBERT AVE NE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
0575	123+79.86	39.00' LT	843.53	432189.70	2468727.07
0576	123+79.86	34.00' LT	843.54	432184.71	2468727.20
0577	123+74.86	39.00' LT	843.46	432189.57	2468722.07
0578	123+74.86	34.00' LT	843.47	432184.57	2468722.20
0579	123+67.86	39.00' LT	842.97	432189.39	2468715.07
0580	123+67.86	34.00' LT	842.98	432184.39	2468715.21
0581	123+65.75	39.00' LT	842.94	432189.33	2468712.97
0582	123+65.71	32.72' LT	843.06	432183.05	2468713.09
0583	123+63.33	38.36' LT	843.02	432188.63	2468710.57
0584	123+70.06	31.00' LT	843.50	432181.45	2468717.49
0585	123+65.20	42.00' LT	843.42	432192.32	2468712.34
0586	123+94.86	39.00' LT	843.93	432190.10	2468742.06
0587	123+94.86	34.00' LT	843.85	432185.10	2468742.20
0588	123+83.00	22.06' LT	843.18	432172.85	2468730.66
0589	123+76.99	23.36' LT	843.15	432173.99	2468724.62
0590	123+62.65	42.54' LT	843.00	432192.79	2468709.77
0591	123+62.64	46.49' LT	842.96	432196.74	2468709.66



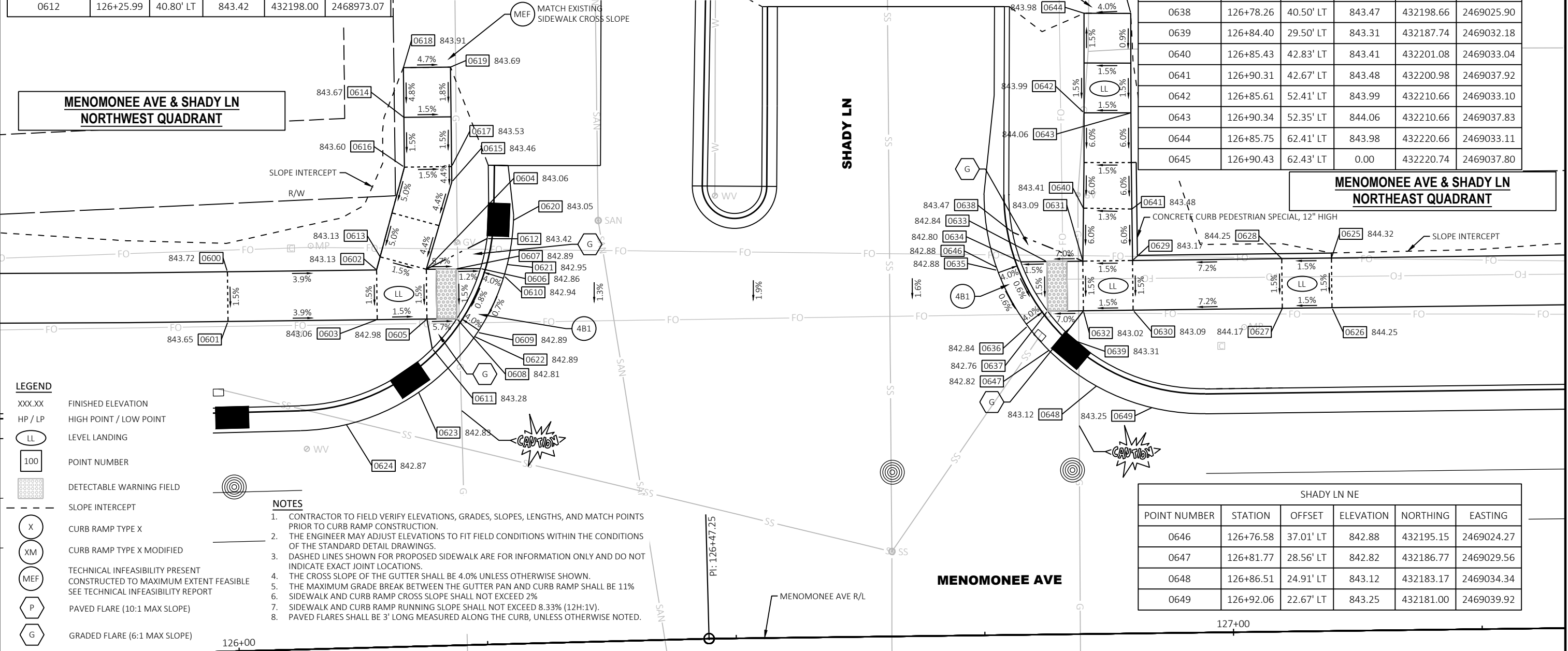
- LEGEND**
- XXX.XX FINISHED ELEVATION
 - HP / LP HIGH POINT / LOW POINT
 - LL LEVEL LANDING
 - 100 POINT NUMBER
 - [Hatched Box] DETECTABLE WARNING FIELD
 - - - SLOPE INTERCEPT
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 - XM CURB RAMP TYPE X MODIFIED
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SHADY LN NW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
0600	125+99.82	38.16' LT	843.72	432194.68	2468946.98
0601	125+99.75	33.17' LT	843.65	432189.68	2468947.04
0602	126+14.82	37.96' LT	843.13	432194.86	2468961.98
0603	126+14.75	32.96' LT	843.06	432189.86	2468962.04
0604	126+19.82	37.89' LT	843.06	432194.93	2468966.98
0605	126+19.75	32.89' LT	842.98	432189.93	2468967.04
0606	126+25.25	37.81' LT	842.86	432194.99	2468972.40
0607	126+22.82	37.84' LT	842.89	432194.96	2468969.98
0608	126+22.75	32.84' LT	842.81	432189.96	2468970.04
0609	126+24.80	31.41' LT	842.89	432188.59	2468972.13
0610	126+27.62	37.02' LT	842.94	432194.26	2468974.79
0611	126+20.20	29.88' LT	843.28	432186.93	2468967.57
0612	126+25.99	40.80' LT	843.42	432198.00	2468973.07

SHADY LN NW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
0613	126+15.21	39.26' LT	843.13	432196.18	2468962.33
0614	126+17.97	53.21' LT	843.67	432210.20	2468964.73
0615	126+22.61	46.60' LT	843.46	432203.71	2468969.54
0616	126+17.88	48.21' LT	843.60	432205.20	2468964.76
0617	126+22.64	48.14' LT	843.53	432205.26	2468969.52
0618	126+18.05	58.21' LT	843.91	432215.20	2468964.67
0619	126+22.79	58.14' LT	843.69	432215.25	2468969.41
0620	126+28.71	42.59' LT	843.05	432199.86	2468975.74
0621	126+27.97	38.16' LT	842.95	432195.41	2468975.11
0622	126+24.50	31.00' LT	842.89	432188.16	2468971.84
0623	126+18.72	25.54' LT	842.83	432182.55	2468966.20
0624	126+11.37	22.49' LT	842.87	432179.31	2468958.93

SHADY LN NE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
0625	127+10.31	37.50' LT	844.32	432196.06	2469057.98
0626	127+10.31	32.50' LT	844.25	432191.06	2469058.04
0627	127+05.31	32.50' LT	844.17	432191.00	2469053.04
0628	127+05.31	37.50' LT	844.25	432196.00	2469052.98
0629	126+90.31	37.50' LT	843.17	432195.81	2469037.98
0630	126+90.31	32.50' LT	843.09	432190.81	2469038.04
0631	126+85.31	37.50' LT	843.09	432195.75	2469032.98
0632	126+85.31	32.50' LT	843.02	432190.75	2469033.05
0633	126+81.69	37.50' LT	842.84	432195.70	2469029.37
0634	126+79.06	37.50' LT	842.80	432195.67	2469026.73
0635	126+76.70	36.67' LT	842.88	432194.81	2469024.39
0636	126+79.68	31.03' LT	842.84	432189.21	2469027.43
0637	126+81.69	32.50' LT	842.76	432190.70	2469029.43
0638	126+78.26	40.50' LT	843.47	432198.66	2469025.90
0639	126+84.40	29.50' LT	843.31	432187.74	2469032.18
0640	126+85.43	42.83' LT	843.41	432201.08	2469033.04
0641	126+90.31	42.67' LT	843.48	432200.98	2469037.92
0642	126+85.61	52.41' LT	843.99	432210.66	2469033.10
0643	126+90.34	52.35' LT	844.06	432210.66	2469037.83
0644	126+85.75	62.41' LT	843.98	432220.66	2469033.11
0645	126+90.43	62.43' LT	0.00	432220.74	2469037.80

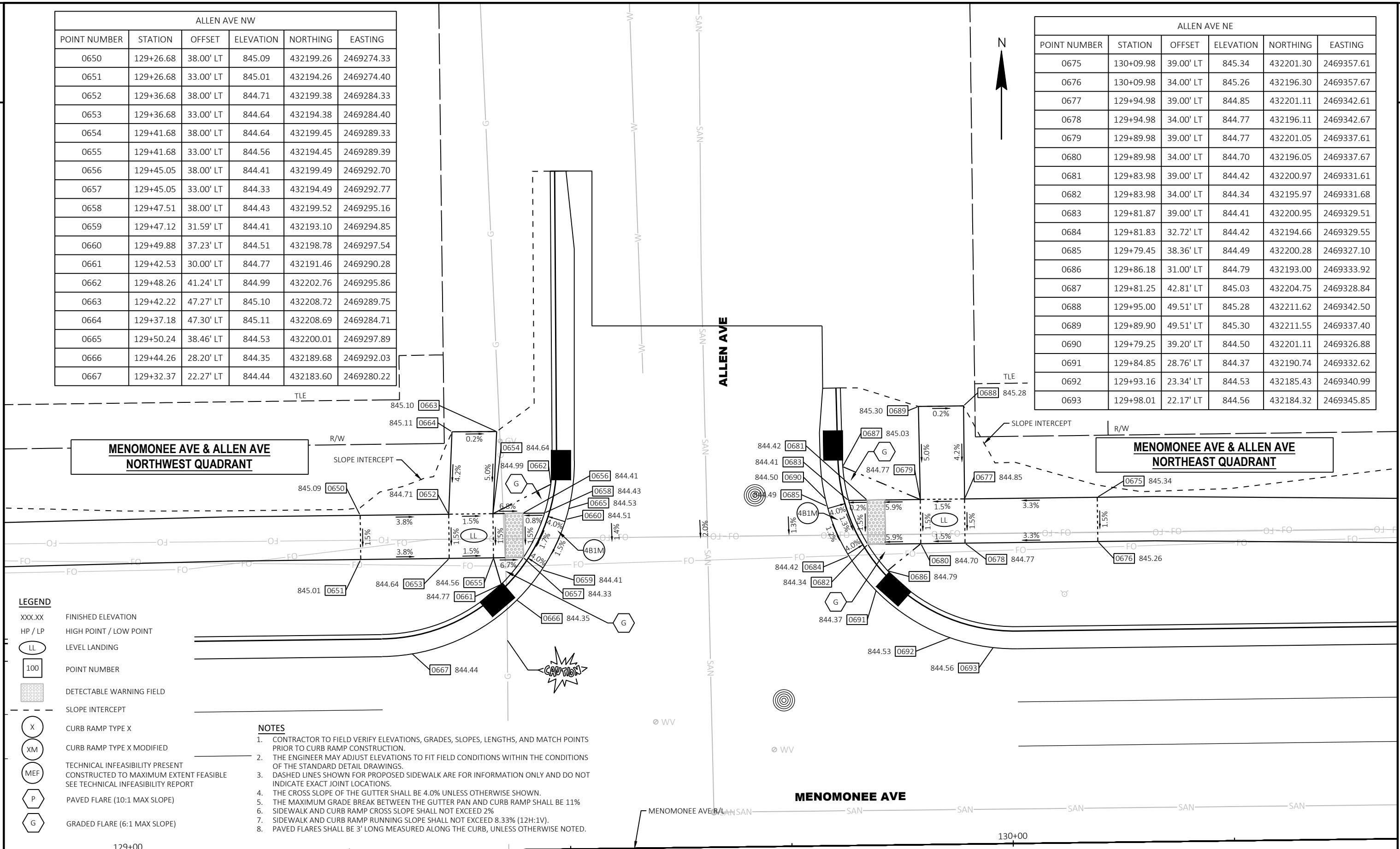


- LEGEND**
- XXX.XX FINISHED ELEVATION
 - HP / LP HIGH POINT / LOW POINT
 - LL LEVEL LANDING
 - 100 POINT NUMBER
 - [Pattern] DETECTABLE WARNING FIELD
 - - - SLOPE INTERCEPT
 - X CURB RAMP TYPE X
 - XM CURB RAMP TYPE X MODIFIED
 - MEF TECHNICAL INFEASIBILITY PRESENT CONSTRUCTED TO MAXIMUM EXTENT FEASIBLE SEE TECHNICAL INFEASIBILITY REPORT
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ALLEN AVE NW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
0650	129+26.68	38.00' LT	845.09	432199.26	2469274.33
0651	129+26.68	33.00' LT	845.01	432194.26	2469274.40
0652	129+36.68	38.00' LT	844.71	432199.38	2469284.33
0653	129+36.68	33.00' LT	844.64	432194.38	2469284.40
0654	129+41.68	38.00' LT	844.64	432199.45	2469289.33
0655	129+41.68	33.00' LT	844.56	432194.45	2469289.39
0656	129+45.05	38.00' LT	844.41	432199.49	2469292.70
0657	129+45.05	33.00' LT	844.33	432194.49	2469292.77
0658	129+47.51	38.00' LT	844.43	432199.52	2469295.16
0659	129+47.12	31.59' LT	844.41	432193.10	2469294.85
0660	129+49.88	37.23' LT	844.51	432198.78	2469297.54
0661	129+42.53	30.00' LT	844.77	432191.46	2469290.28
0662	129+48.26	41.24' LT	844.99	432202.76	2469295.86
0663	129+42.22	47.27' LT	845.10	432208.72	2469289.75
0664	129+37.18	47.30' LT	845.11	432208.69	2469284.71
0665	129+50.24	38.46' LT	844.53	432200.01	2469297.89
0666	129+44.26	28.20' LT	844.35	432189.68	2469292.03
0667	129+32.37	22.27' LT	844.44	432183.60	2469280.22

ALLEN AVE NE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
0675	130+09.98	39.00' LT	845.34	432201.30	2469357.61
0676	130+09.98	34.00' LT	845.26	432196.30	2469357.67
0677	129+94.98	39.00' LT	844.85	432201.11	2469342.61
0678	129+94.98	34.00' LT	844.77	432196.11	2469342.67
0679	129+89.98	39.00' LT	844.77	432201.05	2469337.61
0680	129+89.98	34.00' LT	844.70	432196.05	2469337.67
0681	129+83.98	39.00' LT	844.42	432200.97	2469331.61
0682	129+83.98	34.00' LT	844.34	432195.97	2469331.68
0683	129+81.87	39.00' LT	844.41	432200.95	2469329.51
0684	129+81.83	32.72' LT	844.42	432194.66	2469329.55
0685	129+79.45	38.36' LT	844.49	432200.28	2469327.10
0686	129+86.18	31.00' LT	844.79	432193.00	2469333.92
0687	129+81.25	42.81' LT	845.03	432204.75	2469328.84
0688	129+95.00	49.51' LT	845.28	432211.62	2469342.50
0689	129+89.90	49.51' LT	845.30	432211.55	2469337.40
0690	129+79.25	39.20' LT	844.50	432201.11	2469326.88
0691	129+84.85	28.76' LT	844.37	432190.74	2469332.62
0692	129+93.16	23.34' LT	844.53	432185.43	2469340.99
0693	129+98.01	22.17' LT	844.56	432184.32	2469345.85



- LEGEND**
- XXX.XX FINISHED ELEVATION
 - HP / LP HIGH POINT / LOW POINT
 - LL LEVEL LANDING
 - 100 POINT NUMBER
 - [Pattern] DETECTABLE WARNING FIELD
 - SLOPE INTERCEPT
 - X CURB RAMP TYPE X
 - XM CURB RAMP TYPE X MODIFIED
 - MEF TECHNICAL INFEASIBILITY PRESENT
CONSTRUCTED TO MAXIMUM EXTENT FEASIBLE
SEE TECHNICAL INFEASIBILITY REPORT
 - P PAVED FLARE (10:1 MAX SLOPE)
 - G GRADED FLARE (6:1 MAX SLOPE)

- NOTES**
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 2. THE ENGINEER MAY ADJUST ELEVATIONS TO FIT FIELD CONDITIONS WITHIN THE CONDITIONS OF THE STANDARD DETAIL DRAWINGS.
 3. DASHED LINES SHOWN FOR PROPOSED SIDEWALK ARE FOR INFORMATION ONLY AND DO NOT INDICATE EXACT JOINT LOCATIONS.
 4. THE CROSS SLOPE OF THE GUTTER SHALL BE 4.0% UNLESS OTHERWISE SHOWN.
 5. THE MAXIMUM GRADE BREAK BETWEEN THE GUTTER PAN AND CURB RAMP SHALL BE 11%.
 6. SIDEWALK AND CURB RAMP CROSS SLOPE SHALL NOT EXCEED 2%.
 7. SIDEWALK AND CURB RAMP RUNNING SLOPE SHALL NOT EXCEED 8.33% (12H:1V).
 8. PAVED FLARES SHALL BE 3' LONG MEASURED ALONG THE CURB, UNLESS OTHERWISE NOTED.

LLOYD AVE NW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
0700	132+20.31	37.00' LT	844.24	432201.92	2469567.95
0701	132+20.31	32.00' LT	844.17	432196.92	2469568.01
0702	132+35.31	37.00' LT	843.62	432202.11	2469582.95
0703	132+35.31	32.00' LT	843.55	432197.11	2469583.01
0704	132+40.31	37.00' LT	843.55	432202.17	2469587.95
0705	132+40.31	32.00' LT	843.47	432197.17	2469588.01
0706	132+44.05	37.00' LT	843.32	432202.22	2469591.69
0707	132+44.05	32.00' LT	843.25	432197.22	2469591.75
0708	132+46.88	37.00' LT	843.28	432202.25	2469594.52
0709	132+46.02	30.46' LT	843.33	432195.70	2469593.74
0710	132+49.21	36.10' LT	843.36	432201.38	2469596.86
0711	132+41.14	29.00' LT	843.73	432194.18	2469588.88
0712	132+48.05	41.80' LT	844.01	432207.07	2469595.63
0713	132+35.31	37.39' LT	843.63	432202.50	2469582.95
0714	132+35.37	37.78' LT	843.64	432202.89	2469583.00
0715	132+41.10	41.94' LT	843.90	432207.12	2469588.67
0716	132+36.16	42.72' LT	843.98	432207.84	2469583.73
0717	132+36.94	47.66' LT	844.33	432212.79	2469584.45
0718	132+41.88	46.88' LT	844.25	432212.06	2469589.40
0719	132+42.23	49.10' LT	844.28	432214.29	2469589.72
0720	132+37.36	50.28' LT	844.37	432215.42	2469584.83
0721	132+37.36	51.80' LT	844.39	432216.94	2469584.82
0722	132+42.24	51.79' LT	844.32	432216.98	2469589.70
0723	132+50.68	63.39' LT	843.82	432228.69	2469597.99
0724	132+50.68	46.39' LT	843.64	432211.69	2469598.20

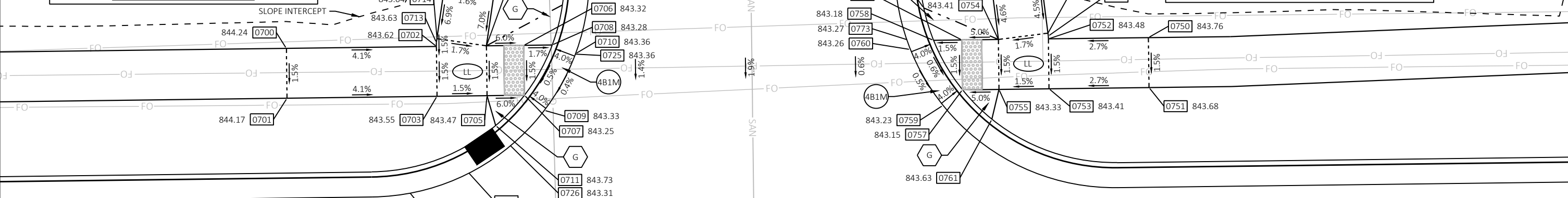
- LEGEND**
- XXX.XX FINISHED ELEVATION
 - HP / LP HIGH POINT / LOW POINT
 - LL LEVEL LANDING
 - 100 POINT NUMBER
 - [Grid Pattern] DETECTABLE WARNING FIELD
 - - - SLOPE INTERCEPT
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LLOYD AVE NE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
0750	133+06.50	37.00' LT	843.76	432202.99	2469654.14
0751	133+06.50	32.00' LT	843.68	432197.99	2469654.20
0752	132+96.50	37.00' LT	843.48	432202.87	2469644.14
0753	132+96.50	32.00' LT	843.41	432197.87	2469644.20
0754	132+91.50	37.00' LT	843.41	432202.81	2469639.14
0755	132+91.50	32.00' LT	843.33	432197.81	2469639.20
0756	132+87.83	37.00' LT	843.22	432202.76	2469635.47
0757	132+87.83	32.00' LT	843.15	432197.76	2469635.53
0758	132+85.01	37.00' LT	843.18	432202.73	2469632.64
0759	132+85.86	30.46' LT	843.23	432196.20	2469633.58
0760	132+82.67	36.10' LT	843.26	432201.80	2469630.32
0761	132+90.75	29.00' LT	843.63	432194.80	2469638.48
0762	132+84.07	40.26' LT	843.73	432205.97	2469631.66
0763	132+96.50	37.29' LT	843.49	432203.16	2469644.13
0764	132+96.47	37.57' LT	843.49	432203.44	2469644.10
0765	132+95.89	42.54' LT	843.72	432208.40	2469643.46
0766	132+95.32	47.51' LT	843.94	432213.36	2469642.82
0767	132+95.03	50.00' LT	844.06	432215.85	2469642.51
0768	132+95.07	54.38' LT	844.26	432220.23	2469642.49
0769	132+90.11	54.43' LT	844.16	432220.22	2469637.53
0770	132+90.07	49.43' LT	843.97	432215.22	2469637.55
0771	132+90.35	46.93' LT	843.87	432212.73	2469637.87
0772	132+90.93	41.97' LT	843.64	432207.77	2469638.50
0773	132+82.31	37.12' LT	843.27	432202.81	2469629.94
0774	132+81.21	51.00' LT	843.47	432216.68	2469628.67

**MENOMONEE AVE & LLOYD AVE
NORTHWEST QUADRANT**

**MENOMONEE AVE & LLOYD AVE
NORTHEAST QUADRANT**



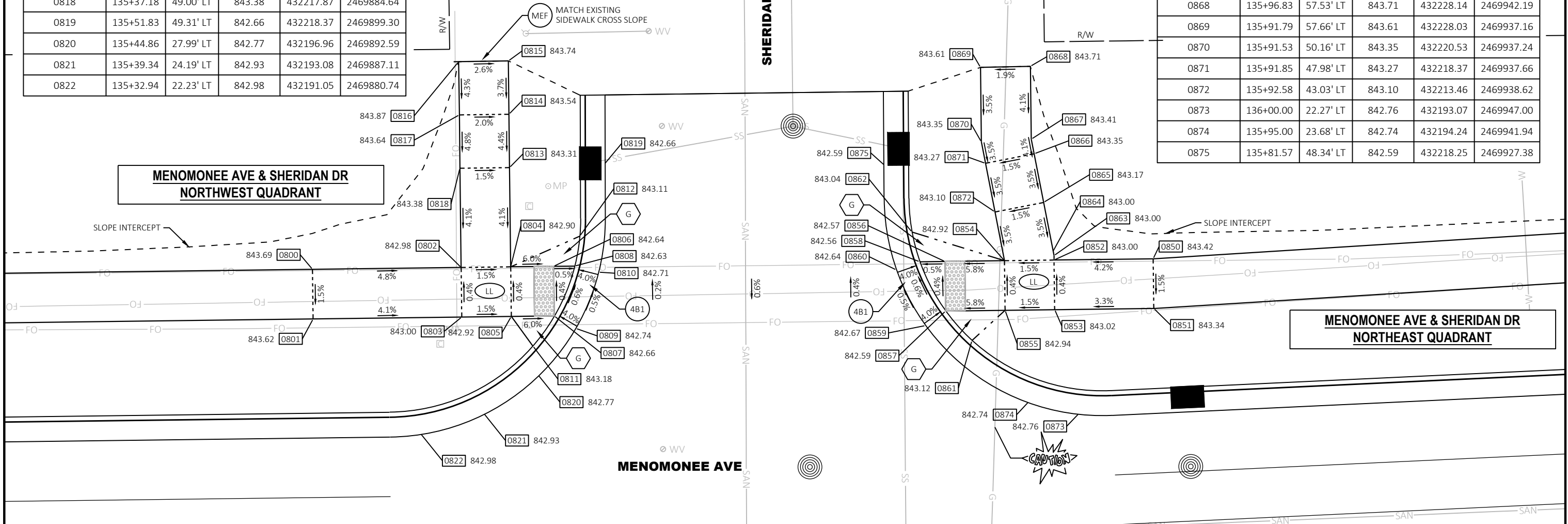
LLOYD AVE NW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
0725	132+49.20	36.06' LT	843.36	432201.34	2469596.85
0726	132+42.60	26.96' LT	843.31	432192.16	2469590.36
0727	132+38.41	24.27' LT	843.46	432189.41	2469586.21
0728	132+32.50	22.34' LT	843.53	432187.41	2469580.33

SHERIDAN DR NW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
0800	135+22.18	39.00' LT	843.69	432207.68	2469869.77
0801	135+22.18	34.00' LT	843.62	432202.68	2469869.83
0802	135+37.18	39.00' LT	842.98	432207.87	2469884.77
0803	135+37.18	34.00' LT	843.00	432202.87	2469884.83
0804	135+42.18	39.00' LT	842.90	432207.93	2469889.77
0805	135+42.18	34.00' LT	842.92	432202.93	2469889.83
0806	135+46.52	39.00' LT	842.64	432207.99	2469894.11
0807	135+46.52	34.00' LT	842.66	432202.99	2469894.17
0808	135+48.62	39.00' LT	842.63	432208.01	2469896.21
0809	135+48.66	32.72' LT	842.74	432201.73	2469896.33
0810	135+51.04	38.36' LT	842.71	432207.40	2469898.64
0811	135+44.31	31.00' LT	843.18	432199.96	2469892.00
0812	135+49.17	42.00' LT	843.11	432211.02	2469896.73
0813	135+42.18	49.00' LT	843.31	432217.93	2469889.64
0814	135+42.19	54.37' LT	843.54	432223.31	2469889.59
0815	135+42.20	59.75' LT	843.74	432228.68	2469889.54
0816	135+37.18	59.73' LT	843.87	432228.60	2469884.51
0817	135+37.19	54.37' LT	843.64	432223.24	2469884.59
0818	135+37.18	49.00' LT	843.38	432217.87	2469884.64
0819	135+51.83	49.31' LT	842.66	432218.37	2469899.30
0820	135+44.86	27.99' LT	842.77	432196.96	2469892.59
0821	135+39.34	24.19' LT	842.93	432193.08	2469887.11
0822	135+32.94	22.23' LT	842.98	432191.05	2469880.74

- LEGEND**
- XXX.XX FINISHED ELEVATION
 - HP / LP HIGH POINT / LOW POINT
 - LL LEVEL LANDING
 - 100 POINT NUMBER
 - [Grid Pattern] DETECTABLE WARNING FIELD
 - - - SLOPE INTERCEPT
 - X CURB RAMP TYPE X
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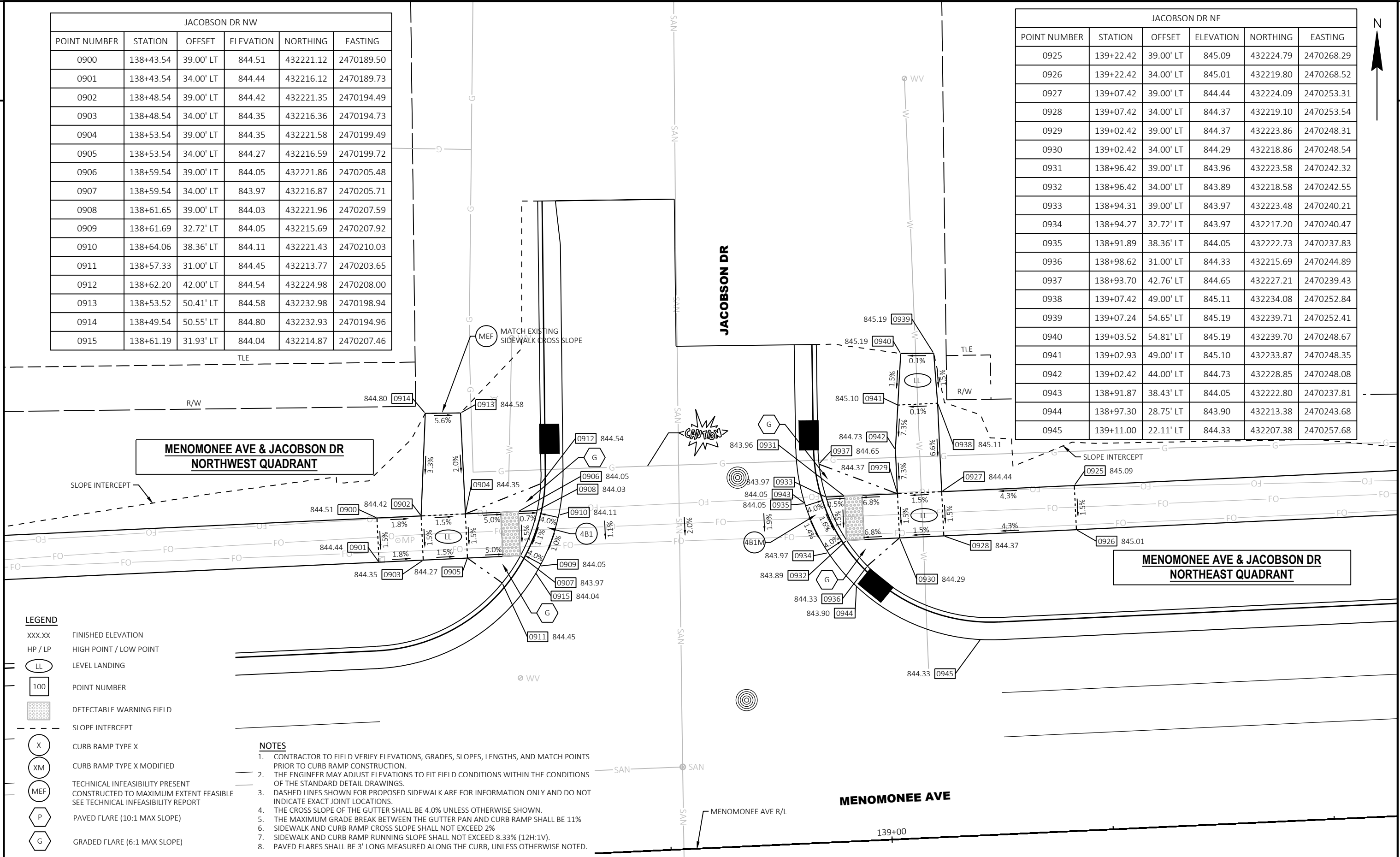
SHERIDAN DR NE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
0850	136+08.30	37.58' LT	843.42	432208.74	2469954.58
0851	136+08.13	32.58' LT	843.34	432203.74	2469954.64
0852	135+98.30	37.92' LT	843.00	432208.62	2469944.58
0853	135+98.13	32.92' LT	843.02	432203.62	2469944.64
0854	135+93.31	38.09' LT	842.92	432208.55	2469939.58
0855	135+93.13	33.09' LT	842.94	432203.55	2469939.64
0856	135+87.31	38.29' LT	842.57	432208.48	2469933.58
0857	135+87.14	33.30' LT	842.59	432203.48	2469933.64
0858	135+84.77	38.38' LT	842.56	432208.45	2469931.03
0859	135+85.05	31.92' LT	842.67	432202.01	2469931.62
0860	135+82.37	37.66' LT	842.64	432207.62	2469928.68
0861	135+89.65	30.21' LT	843.12	432200.51	2469936.30
0862	135+84.11	41.40' LT	843.04	432211.44	2469930.24
0863	135+98.32	38.37' LT	843.00	432209.07	2469944.57
0864	135+98.25	38.81' LT	843.00	432209.51	2469944.49
0865	135+97.53	43.76' LT	843.17	432214.42	2469943.53
0866	135+96.80	48.71' LT	843.35	432219.33	2469942.57
0867	135+96.58	50.20' LT	843.41	432220.80	2469942.28
0868	135+96.83	57.53' LT	843.71	432228.14	2469942.19
0869	135+91.79	57.66' LT	843.61	432228.03	2469937.16
0870	135+91.53	50.16' LT	843.35	432220.53	2469937.24
0871	135+91.85	47.98' LT	843.27	432218.37	2469937.66
0872	135+92.58	43.03' LT	843.10	432213.46	2469938.62
0873	136+00.00	22.27' LT	842.76	432193.07	2469947.00
0874	135+95.00	23.68' LT	842.74	432194.24	2469941.94
0875	135+81.57	48.34' LT	842.59	432218.25	2469927.38



PROJECT NO: 2720-07-71	HWY: MENOMONEE AVE	COUNTY: WAUKESHA	CURB RAMP DETAILS	SHEET	E
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JACOBSON DR NW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
0900	138+43.54	39.00' LT	844.51	432221.12	2470189.50
0901	138+43.54	34.00' LT	844.44	432216.12	2470189.73
0902	138+48.54	39.00' LT	844.42	432221.35	2470194.49
0903	138+48.54	34.00' LT	844.35	432216.36	2470194.73
0904	138+53.54	39.00' LT	844.35	432221.58	2470199.49
0905	138+53.54	34.00' LT	844.27	432216.59	2470199.72
0906	138+59.54	39.00' LT	844.05	432221.86	2470205.48
0907	138+59.54	34.00' LT	843.97	432216.87	2470205.71
0908	138+61.65	39.00' LT	844.03	432221.96	2470207.59
0909	138+61.69	32.72' LT	844.05	432215.69	2470207.92
0910	138+64.06	38.36' LT	844.11	432221.43	2470210.03
0911	138+57.33	31.00' LT	844.45	432213.77	2470203.65
0912	138+62.20	42.00' LT	844.54	432224.98	2470208.00
0913	138+53.52	50.41' LT	844.58	432232.98	2470198.94
0914	138+49.54	50.55' LT	844.80	432232.93	2470194.96
0915	138+61.19	31.93' LT	844.04	432214.87	2470207.46

JACOBSON DR NE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
0925	139+22.42	39.00' LT	845.09	432224.79	2470268.29
0926	139+22.42	34.00' LT	845.01	432219.80	2470268.52
0927	139+07.42	39.00' LT	844.44	432224.09	2470253.31
0928	139+07.42	34.00' LT	844.37	432219.10	2470253.54
0929	139+02.42	39.00' LT	844.37	432223.86	2470248.31
0930	139+02.42	34.00' LT	844.29	432218.86	2470248.54
0931	138+96.42	39.00' LT	843.96	432223.58	2470242.32
0932	138+96.42	34.00' LT	843.89	432218.58	2470242.55
0933	138+94.31	39.00' LT	843.97	432223.48	2470240.21
0934	138+94.27	32.72' LT	843.97	432217.20	2470240.47
0935	138+91.89	38.36' LT	844.05	432222.73	2470237.83
0936	138+98.62	31.00' LT	844.33	432215.69	2470244.89
0937	138+93.70	42.76' LT	844.65	432227.21	2470239.43
0938	139+07.42	49.00' LT	845.11	432234.08	2470252.84
0939	139+07.24	54.65' LT	845.19	432239.71	2470252.41
0940	139+03.52	54.81' LT	845.19	432239.70	2470248.67
0941	139+02.93	49.00' LT	845.10	432233.87	2470248.35
0942	139+02.42	44.00' LT	844.73	432228.85	2470248.08
0943	138+91.87	38.43' LT	844.05	432222.80	2470237.81
0944	138+97.30	28.75' LT	843.90	432213.38	2470243.68
0945	139+11.00	22.11' LT	844.33	432207.38	2470257.68



**MENOMONEE AVE & JACOBSON DR
NORTHWEST QUADRANT**

**MENOMONEE AVE & JACOBSON DR
NORTHEAST QUADRANT**

- LEGEND**
- XXX.XX FINISHED ELEVATION
 - HP / LP HIGH POINT / LOW POINT
 - LL LEVEL LANDING
 - 100 POINT NUMBER
 - [Pattern] DETECTABLE WARNING FIELD
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 - XM CURB RAMP TYPE X MODIFIED
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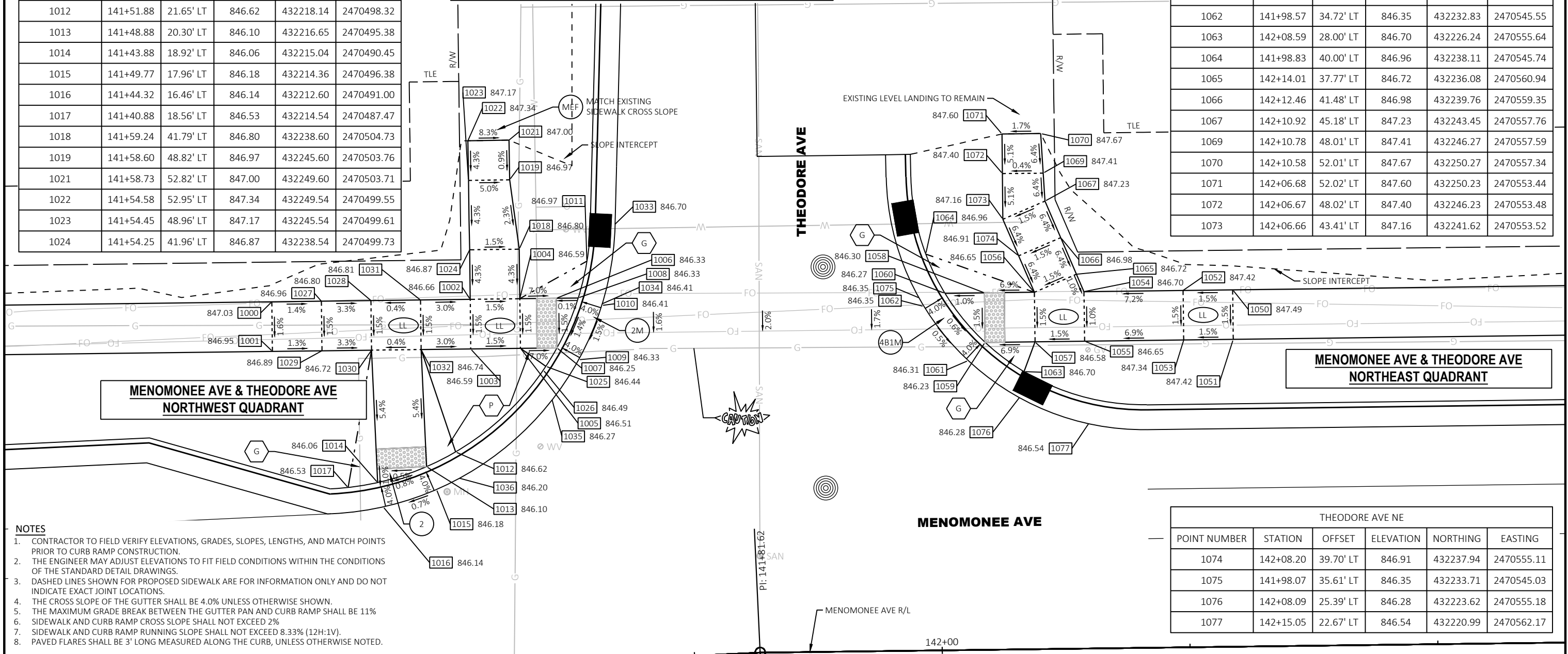
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THEODORE AVE NW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1000	141+34.06	37.64' LT	847.03	432233.29	2470479.77
1001	141+33.89	32.65' LT	846.95	432228.29	2470479.83
1002	141+54.07	36.96' LT	846.66	432233.54	2470499.79
1003	141+53.88	31.96' LT	846.59	432228.54	2470499.83
1004	141+59.07	36.79' LT	846.59	432233.60	2470504.79
1005	141+58.90	31.79' LT	846.51	432228.60	2470504.85
1006	141+62.74	36.67' LT	846.33	432233.65	2470508.47
1007	141+62.57	31.67' LT	846.25	432228.65	2470508.53
1008	141+64.94	36.59' LT	846.33	432233.68	2470510.67
1009	141+64.71	30.37' LT	846.33	432227.45	2470510.72
1010	141+67.29	35.73' LT	846.41	432232.93	2470513.05
1011	141+66.00	40.25' LT	846.97	432237.38	2470511.55
1012	141+51.88	21.65' LT	846.62	432218.14	2470498.32
1013	141+48.88	20.30' LT	846.10	432216.65	2470495.38
1014	141+43.88	18.92' LT	846.06	432215.04	2470490.45
1015	141+49.77	17.96' LT	846.18	432214.36	2470496.38
1016	141+44.32	16.46' LT	846.14	432212.60	2470491.00
1017	141+40.88	18.56' LT	846.53	432214.54	2470487.47
1018	141+59.24	41.79' LT	846.80	432238.60	2470504.73
1019	141+58.60	48.82' LT	846.97	432245.60	2470503.76
1021	141+58.73	52.82' LT	847.00	432249.60	2470503.71
1022	141+54.58	52.95' LT	847.34	432249.54	2470499.55
1023	141+54.45	48.96' LT	847.17	432245.54	2470499.61
1024	141+54.25	41.96' LT	846.87	432238.54	2470499.73

THEODORE AVE NW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1025	141+61.37	29.86' LT	846.44	432226.79	2470507.41
1026	141+61.10	29.49' LT	846.49	432226.41	2470507.15
1027	141+39.06	37.47' LT	846.96	432233.35	2470484.77
1028	141+44.06	37.30' LT	846.80	432233.42	2470489.77
1029	141+38.89	32.48' LT	846.89	432228.35	2470484.83
1030	141+43.89	32.31' LT	846.72	432228.42	2470489.83
1031	141+49.06	37.13' LT	846.81	432233.48	2470494.77
1032	141+48.88	32.14' LT	846.74	432228.48	2470494.83
1033	141+69.03	43.86' LT	846.70	432241.13	2470514.41
1034	141+67.35	35.89' LT	846.41	432233.08	2470513.10
1035	141+61.84	26.43' LT	846.27	432223.38	2470508.04
1036	141+52.15	18.98' LT	846.20	432215.49	2470498.70

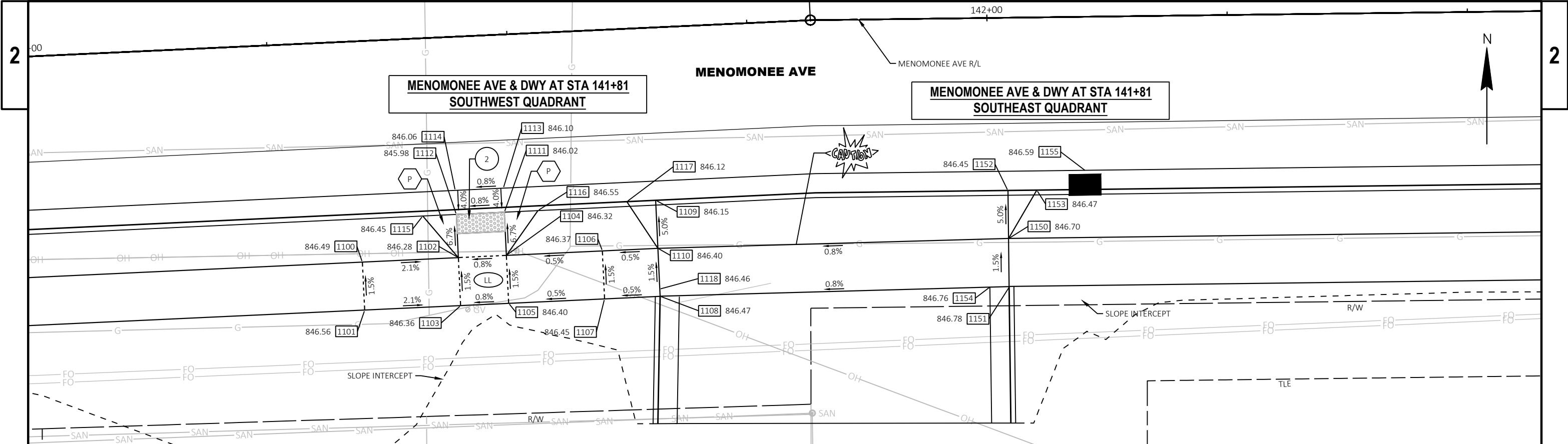
THEODORE AVE NE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1050	142+29.75	36.00' LT	847.49	432234.50	2470576.70
1051	142+29.75	31.00' LT	847.42	432229.50	2470576.76
1052	142+24.75	36.00' LT	847.42	432234.44	2470571.70
1053	142+24.75	31.00' LT	847.34	432229.44	2470571.76
1054	142+14.75	36.00' LT	846.70	432234.31	2470561.70
1055	142+14.75	31.00' LT	846.65	432229.31	2470561.76
1056	142+09.75	36.00' LT	846.65	432234.25	2470556.70
1057	142+09.75	31.00' LT	846.58	432229.25	2470556.76
1058	142+04.70	36.00' LT	846.30	432234.19	2470551.66
1059	142+04.70	31.00' LT	846.23	432229.19	2470551.72
1060	142+00.72	36.00' LT	846.27	432234.14	2470547.68
1061	142+02.98	29.19' LT	846.31	432227.36	2470550.02
1062	141+98.57	34.72' LT	846.35	432232.83	2470545.55
1063	142+08.59	28.00' LT	846.70	432226.24	2470555.64
1064	141+98.83	40.00' LT	846.96	432238.11	2470545.74
1065	142+14.01	37.77' LT	846.72	432236.08	2470560.94
1066	142+12.46	41.48' LT	846.98	432239.76	2470559.35
1067	142+10.92	45.18' LT	847.23	432243.45	2470557.76
1069	142+10.78	48.01' LT	847.41	432246.27	2470557.59
1070	142+10.58	52.01' LT	847.67	432250.27	2470557.34
1071	142+06.68	52.02' LT	847.60	432250.23	2470553.44
1072	142+06.67	48.02' LT	847.40	432246.23	2470553.48
1073	142+06.66	43.41' LT	847.16	432241.62	2470553.52

- LEGEND**
- XXX.XX FINISHED ELEVATION
 - HP / LP HIGH POINT / LOW POINT
 - LL LEVEL LANDING
 - 100 POINT NUMBER
 - [Pattern] DETECTABLE WARNING FIELD
 - - - SLOPE INTERCEPT
 - X CURB RAMP TYPE X
 - XM CURB RAMP TYPE X MODIFIED
 - MEF TECHNICAL INFEASIBILITY PRESENT CONSTRUCTED TO MAXIMUM EXTENT FEASIBLE SEE TECHNICAL INFEASIBILITY REPORT
 - P PAVED FLARE (10:1 MAX SLOPE)
 - G GRADED FLARE (6:1 MAX SLOPE)



- NOTES**
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 - PAVED FLARES SHALL BE 3' LONG MEASURED ALONG THE CURB, UNLESS OTHERWISE NOTED.

THEODORE AVE NE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1074	142+08.20	39.70' LT	846.91	432237.94	2470555.11
1075	141+98.07	35.61' LT	846.35	432233.71	2470545.03
1076	142+08.09	25.39' LT	846.28	432223.62	2470555.18
1077	142+15.05	22.67' LT	846.54	432220.99	2470562.17



DRIVEWAY AT STA 141+81 SW

POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1100	141+33.88	23.00' RT	846.49	432172.70	2470482.42
1101	141+33.88	28.00' RT	846.56	432167.71	2470482.65
1102	141+43.88	23.00' RT	846.28	432173.17	2470492.40
1103	141+43.88	28.00' RT	846.36	432168.18	2470492.64
1104	141+48.88	23.00' RT	846.32	432173.40	2470497.40
1105	141+48.88	28.00' RT	846.40	432168.41	2470497.63
1106	141+58.88	23.00' RT	846.37	432173.87	2470507.39
1107	141+58.88	28.00' RT	846.45	432168.87	2470507.62
1108	141+64.67	28.00' RT	846.47	432169.14	2470513.40
1109	141+64.67	18.00' RT	846.15	432179.13	2470512.93
1110	141+64.67	23.00' RT	846.40	432174.14	2470513.17
1111	141+48.88	18.50' RT	846.02	432177.90	2470497.19
1112	141+43.88	18.50' RT	845.98	432177.67	2470492.20
1113	141+48.88	16.00' RT	846.10	432180.40	2470497.07
1114	141+43.88	16.00' RT	846.06	432180.16	2470492.08
1115	141+40.38	18.50' RT	846.45	432177.50	2470488.70
1116	141+52.38	18.50' RT	846.55	432178.06	2470500.69
1117	141+61.67	18.00' RT	846.12	432178.99	2470509.94
1118	141+64.75	27.23' RT	846.46	432169.92	2470513.45

DRIVEWAY AT STA 141+81 SE

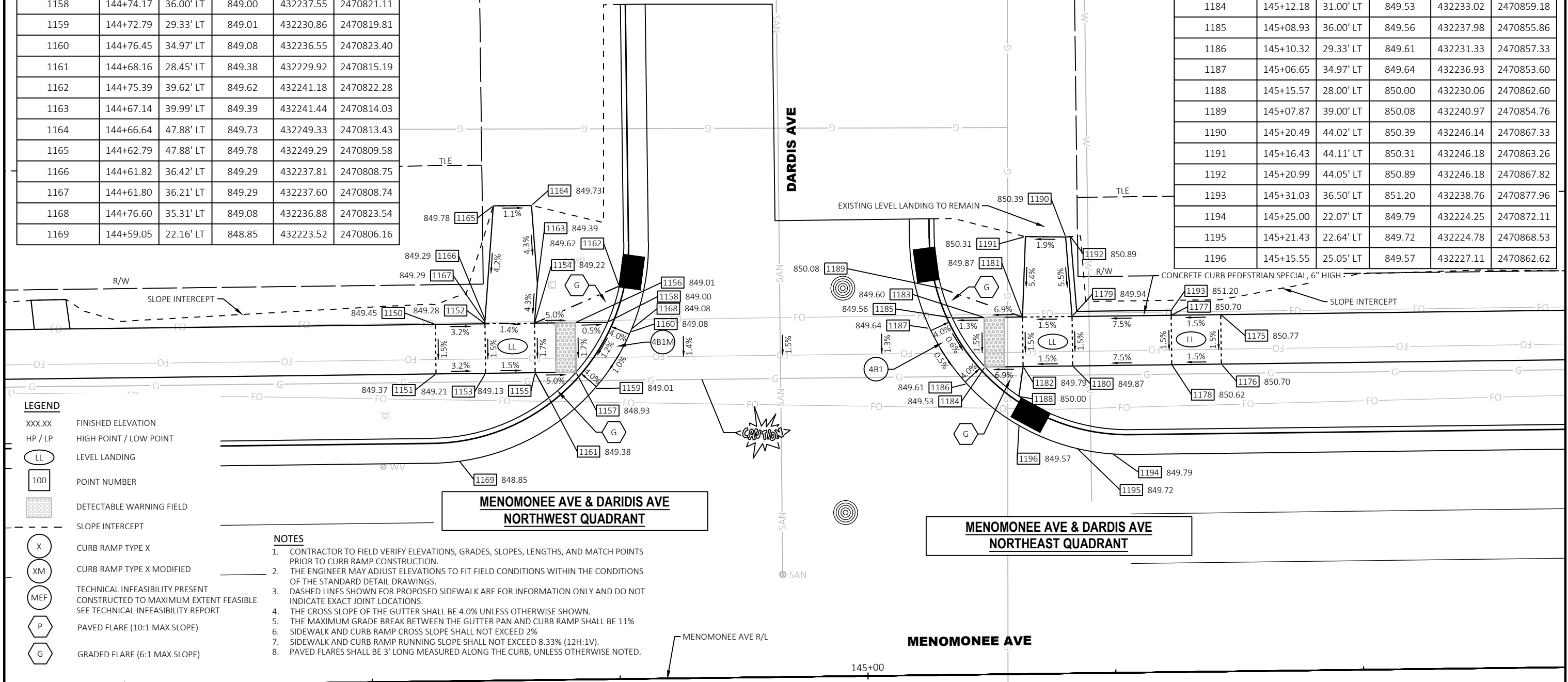
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1150	142+01.97	23.00' RT	846.70	432175.16	2470549.66
1151	142+02.00	28.00' RT	846.78	432170.16	2470549.76
1152	142+01.97	18.00' RT	846.45	432180.16	2470549.60
1153	142+04.97	18.00' RT	846.47	432180.19	2470552.60
1154	141+99.97	28.03' RT	846.76	432170.10	2470547.72
1155	142+10.00	16.00' RT	846.59	432182.26	2470557.60

- LEGEND**
- xxx.xx FINISHED ELEVATION
 - HP / LP HIGH POINT / LOW POINT
 - LL LEVEL LANDING
 - 100 POINT NUMBER
 - [Pattern] DETECTABLE WARNING FIELD
 - - - SLOPE INTERCEPT
 - (X) CURB RAMP TYPE X
 - (XM) CURB RAMP TYPE X MODIFIED
 - (MEF) TECHNICAL INFEASIBILITY PRESENT
CONSTRUCTED TO MAXIMUM EXTENT FEASIBLE
SEE TECHNICAL INFEASIBILITY REPORT
 - (P) PAVED FLARE (10:1 MAX SLOPE)
 - (G) GRADED FLARE (6:1 MAX SLOPE)

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 - PAVED FLARES SHALL BE 3' LONG MEASURED ALONG THE CURB, UNLESS OTHERWISE NOTED.

DARDIS AVE NW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1150	144+56.80	36.00' LT	849.45	432237.33	2470803.74
1151	144+56.80	31.00' LT	849.37	432232.33	2470803.80
1152	144+61.80	36.00' LT	849.28	432237.39	2470808.74
1153	144+61.80	31.00' LT	849.21	432232.39	2470808.80
1154	144+66.80	36.00' LT	849.22	432237.46	2470813.74
1155	144+66.80	31.00' LT	849.13	432232.46	2470813.80
1156	144+70.92	36.00' LT	849.01	432237.51	2470817.86
1157	144+70.92	31.00' LT	848.93	432232.51	2470817.92
1158	144+74.17	36.00' LT	849.00	432237.55	2470821.11
1159	144+72.79	29.33' LT	849.01	432230.86	2470819.81
1160	144+76.45	34.97' LT	849.08	432236.55	2470823.40
1161	144+68.16	28.45' LT	849.38	432229.92	2470815.19
1162	144+75.39	39.62' LT	849.62	432241.18	2470822.28
1163	144+67.14	39.99' LT	849.39	432241.44	2470814.03
1164	144+66.64	47.88' LT	849.73	432249.33	2470813.43
1165	144+62.79	47.88' LT	849.78	432249.29	2470809.58
1166	144+61.82	36.42' LT	849.29	432237.81	2470808.75
1167	144+61.80	36.21' LT	849.29	432237.60	2470808.74
1168	144+76.60	35.31' LT	849.08	432236.88	2470823.54
1169	144+59.05	22.16' LT	848.85	432223.52	2470806.16

DARDIS AVE NE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1175	145+36.03	36.00' LT	850.77	432238.32	2470882.97
1176	145+36.03	31.00' LT	850.70	432233.32	2470883.03
1177	145+31.03	36.00' LT	850.70	432238.26	2470877.97
1178	145+31.03	31.00' LT	850.62	432233.26	2470878.03
1179	145+21.03	36.00' LT	849.94	432238.13	2470867.97
1180	145+21.03	31.00' LT	849.87	432233.13	2470868.03
1181	145+16.03	36.00' LT	849.87	432238.07	2470862.97
1182	145+16.03	31.00' LT	849.79	432233.07	2470863.03
1183	145+12.18	36.00' LT	849.60	432238.02	2470859.11
1184	145+12.18	31.00' LT	849.53	432233.02	2470859.18
1185	145+08.93	36.00' LT	849.56	432237.98	2470855.86
1186	145+10.32	29.33' LT	849.61	432231.33	2470857.33
1187	145+06.65	34.97' LT	849.64	432236.93	2470853.60
1188	145+15.57	28.00' LT	850.00	432230.06	2470862.60
1189	145+07.87	39.00' LT	850.08	432240.97	2470854.76
1190	145+20.49	44.02' LT	850.39	432246.14	2470867.33
1191	145+16.43	44.11' LT	850.31	432246.18	2470863.26
1192	145+20.99	44.05' LT	850.89	432246.18	2470867.82
1193	145+31.03	36.50' LT	851.20	432238.76	2470877.96
1194	145+25.00	22.07' LT	849.79	432224.25	2470872.11
1195	145+21.43	22.64' LT	849.72	432224.78	2470868.53
1196	145+15.55	25.05' LT	849.57	432227.11	2470862.62



- LEGEND**
- XXX.XX FINISHED ELEVATION
 - HP / LP HIGH POINT / LOW POINT
 - LL LEVEL LANDING
 - 100 POINT NUMBER
 - [Pattern] DETECTABLE WARNING FIELD
 - - - SLOPE INTERCEPT
 - X CURB RAMP TYPE X
 - XM CURB RAMP TYPE X MODIFIED
 - MEF TECHNICAL INFEASIBILITY PRESENT CONSTRUCTED TO MAXIMUM EXTENT FEASIBLE SEE TECHNICAL INFEASIBILITY REPORT
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LAVERGNE AVE NW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1124	147+84.28	19.85' LT	854.35	432225.27	2471131.39
1125	147+71.84	16.00' LT	854.25	432221.26	2471119.00
1200	147+63.10	36.00' LT	854.83	432241.15	2471110.01
1201	147+63.10	31.00' LT	854.76	432236.15	2471110.08
1202	147+68.10	36.00' LT	854.76	432241.21	2471115.01
1203	147+68.10	31.00' LT	854.71	432236.21	2471115.08
1204	147+73.10	36.00' LT	854.69	432241.27	2471120.01
1205	147+73.10	31.00' LT	854.65	432236.27	2471120.08
1206	147+78.10	36.00' LT	854.61	432241.34	2471125.01
1207	147+78.10	31.00' LT	854.60	432236.34	2471125.08
1208	147+83.10	36.00' LT	854.54	432241.40	2471130.01
1209	147+83.10	31.00' LT	854.55	432236.40	2471130.08
1210	147+90.04	36.00' LT	854.19	432241.49	2471136.95
1211	147+90.04	31.00' LT	854.20	432236.49	2471137.02
1212	147+91.24	36.00' LT	854.17	432241.50	2471138.15
1213	147+92.37	30.10' LT	854.28	432235.62	2471139.36
1214	147+93.72	35.74' LT	854.25	432241.27	2471140.64
1215	147+88.58	28.00' LT	854.72	432233.47	2471135.59
1216	147+91.35	39.00' LT	854.66	432244.50	2471138.22
1217	147+73.10	18.54' LT	854.18	432223.82	2471120.23
1218	147+68.10	18.50' LT	854.13	432223.71	2471115.23
1219	147+73.26	16.05' LT	854.26	432221.32	2471120.42
1220	147+68.10	16.00' LT	854.21	432221.21	2471115.26
1221	147+76.80	19.14' LT	854.71	432224.46	2471123.92

LAVERGNE AVE NW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1222	147+65.10	19.50' LT	854.58	432224.68	2471112.22
1223	147+82.03	43.70' LT	854.58	432249.08	2471128.85
1224	147+78.01	43.75' LT	854.77	432249.09	2471124.83
1225	147+68.10	20.54' LT	854.22	432225.75	2471115.21
1226	147+73.10	20.54' LT	854.27	432225.82	2471120.21

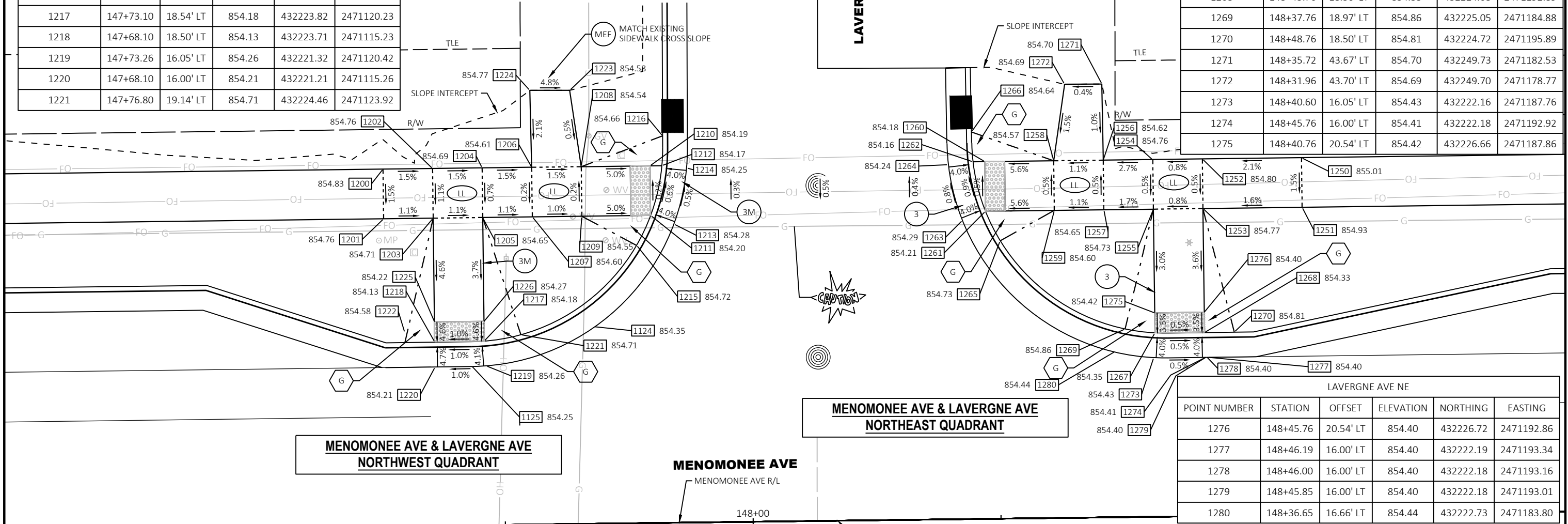
LAVERGNE AVE NE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1250	148+55.76	36.00' LT	855.01	432242.30	2471202.67
1251	148+55.76	31.00' LT	854.93	432237.31	2471202.73
1252	148+45.76	36.00' LT	854.80	432242.18	2471192.67
1253	148+45.76	31.00' LT	854.77	432237.18	2471192.73
1254	148+40.76	36.00' LT	854.76	432242.12	2471187.67
1255	148+40.76	31.00' LT	854.73	432237.12	2471187.73
1256	148+35.76	36.00' LT	854.62	432242.06	2471182.67
1257	148+35.76	31.00' LT	854.65	432237.06	2471182.73
1258	148+30.76	36.00' LT	854.57	432241.99	2471177.67
1259	148+30.76	31.00' LT	854.60	432236.99	2471177.73
1260	148+23.81	36.00' LT	854.18	432241.91	2471170.72
1261	148+23.81	31.00' LT	854.21	432236.91	2471170.78
1262	148+22.61	36.00' LT	854.16	432241.89	2471169.52
1263	148+21.48	30.10' LT	854.29	432235.98	2471168.46
1264	148+20.13	35.74' LT	854.24	432241.60	2471167.04
1265	148+25.27	28.00' LT	854.73	432233.93	2471172.28
1266	148+22.51	39.00' LT	854.64	432244.89	2471169.38
1267	148+40.76	18.54' LT	854.35	432224.66	2471187.89
1268	148+45.76	18.50' LT	854.33	432224.68	2471192.89
1269	148+37.76	18.97' LT	854.86	432225.05	2471184.88
1270	148+48.76	18.50' LT	854.81	432224.72	2471195.89
1271	148+35.72	43.67' LT	854.70	432249.73	2471182.53
1272	148+31.96	43.70' LT	854.69	432249.70	2471178.77
1273	148+40.60	16.05' LT	854.43	432222.16	2471187.76
1274	148+45.76	16.00' LT	854.41	432222.18	2471192.92
1275	148+40.76	20.54' LT	854.42	432226.66	2471187.86

LEGEND

- XXX.XX FINISHED ELEVATION
- HP / LP HIGH POINT / LOW POINT
- LL LEVEL LANDING
- 100 POINT NUMBER
- [Pattern] DETECTABLE WARNING FIELD
- - - SLOPE INTERCEPT
- X CURB RAMP TYPE X
- XM CURB RAMP TYPE X MODIFIED
- MEF TECHNICAL INFEASIBILITY PRESENT
CONSTRUCTED TO MAXIMUM EXTENT FEASIBLE
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- P PAVED FLARE (10:1 MAX SLOPE)
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NOTES

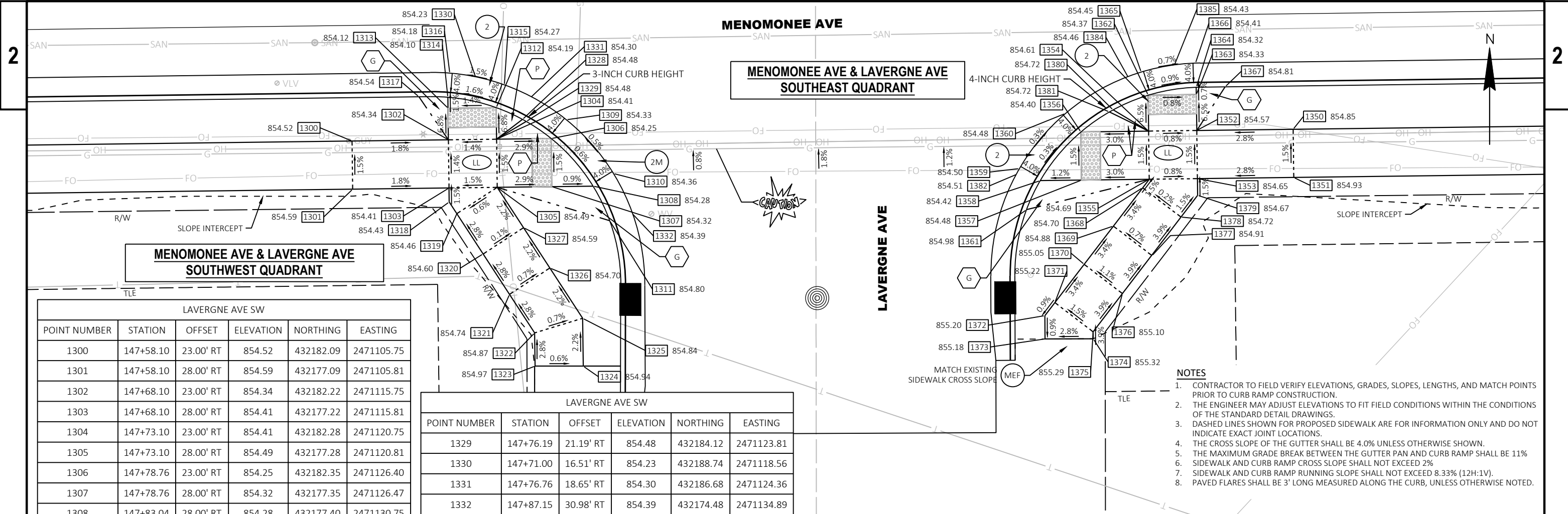
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MENOMONEE AVE & LAVERGNE AVE
NORTHWEST QUADRANT

MENOMONEE AVE & LAVERGNE AVE
NORTHEAST QUADRANT

LAVERGNE AVE NE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1276	148+45.76	20.54' LT	854.40	432226.72	2471192.86
1277	148+46.19	16.00' LT	854.40	432222.19	2471193.34
1278	148+46.00	16.00' LT	854.40	432222.18	2471193.16
1279	148+45.85	16.00' LT	854.40	432222.18	2471193.01
1280	148+36.65	16.66' LT	854.44	432222.73	2471183.80



**MENOMONEE AVE & LAVERGNE AVE
SOUTHWEST QUADRANT**

**MENOMONEE AVE & LAVERGNE AVE
SOUTHEAST QUADRANT**

LAVERGNE AVE SW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1300	147+58.10	23.00' RT	854.52	432182.09	2471105.75
1301	147+58.10	28.00' RT	854.59	432177.09	2471105.81
1302	147+68.10	23.00' RT	854.34	432182.22	2471115.75
1303	147+68.10	28.00' RT	854.41	432177.22	2471115.81
1304	147+73.10	23.00' RT	854.41	432182.28	2471120.75
1305	147+73.10	28.00' RT	854.49	432177.28	2471120.81
1306	147+78.76	23.00' RT	854.25	432182.35	2471126.40
1307	147+78.76	28.00' RT	854.32	432177.35	2471126.47
1308	147+83.04	28.00' RT	854.28	432177.40	2471130.75
1309	147+80.35	21.08' RT	854.33	432184.29	2471127.98
1310	147+85.18	26.72' RT	854.36	432178.71	2471132.88
1311	147+84.72	31.60' RT	854.80	432173.82	2471132.47
1312	147+73.10	19.73' RT	854.19	432185.55	2471120.71
1313	147+68.10	19.73' RT	854.12	432185.49	2471115.71
1314	147+68.10	18.58' RT	854.10	432186.63	2471115.69
1315	147+73.97	17.38' RT	854.27	432187.91	2471121.55
1316	147+68.33	16.09' RT	854.18	432189.12	2471115.89
1317	147+65.10	18.50' RT	854.54	432186.68	2471112.69
1318	147+68.10	29.45' RT	854.43	432175.76	2471115.83
1319	147+68.88	30.68' RT	854.46	432174.55	2471116.62
1320	147+71.56	34.90' RT	854.60	432170.36	2471119.35
1321	147+74.24	39.12' RT	854.74	432166.17	2471122.09
1322	147+76.80	43.16' RT	854.87	432162.17	2471124.70
1323	147+76.80	46.61' RT	854.97	432158.72	2471124.74
1324	147+81.80	46.67' RT	854.94	432158.72	2471129.74
1325	147+81.80	41.71' RT	854.84	432163.68	2471129.68
1326	147+78.46	36.44' RT	854.70	432168.90	2471126.27
1327	147+75.78	32.22' RT	854.59	432173.09	2471123.54
1328	147+75.80	20.97' RT	854.48	432184.34	2471123.43

LAVERGNE AVE SW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1329	147+76.19	21.19' RT	854.48	432184.12	2471123.81
1330	147+71.00	16.51' RT	854.23	432188.74	2471118.56
1331	147+76.76	18.65' RT	854.30	432186.68	2471124.36
1332	147+87.15	30.98' RT	854.39	432174.48	2471134.89

LAVERGNE AVE SE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1350	148+55.76	23.00' RT	854.85	432183.31	2471203.41
1351	148+55.76	28.00' RT	854.93	432178.31	2471203.47
1352	148+45.76	23.00' RT	854.57	432183.18	2471193.41
1353	148+45.76	28.00' RT	854.65	432178.18	2471193.47
1354	148+40.76	23.00' RT	854.61	432183.12	2471188.41
1355	148+40.76	28.00' RT	854.69	432178.12	2471188.47
1356	148+33.73	23.00' RT	854.40	432183.03	2471181.38
1357	148+33.73	28.00' RT	854.48	432178.03	2471181.44
1358	148+29.45	28.00' RT	854.42	432177.98	2471177.16
1359	148+27.31	26.72' RT	854.50	432179.24	2471175.00
1360	148+32.14	21.08' RT	854.48	432184.94	2471179.76
1361	148+27.99	31.00' RT	854.98	432174.96	2471175.74
1362	148+40.76	19.27' RT	854.37	432186.85	2471188.36
1363	148+45.76	19.27' RT	854.33	432186.91	2471193.36
1364	148+45.76	18.50' RT	854.32	432187.68	2471193.35
1365	148+40.07	16.87' RT	854.45	432189.24	2471187.63
1366	148+45.71	16.01' RT	854.41	432190.18	2471193.26
1367	148+48.76	18.50' RT	854.81	432187.72	2471196.35

LAVERGNE AVE SE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1368	148+40.07	28.88' RT	854.70	432177.23	2471187.79
1369	148+36.98	32.82' RT	854.88	432173.26	2471184.75
1370	148+33.90	36.75' RT	855.05	432169.29	2471181.71
1371	148+30.81	40.69' RT	855.22	432165.31	2471178.68
1372	148+29.76	42.03' RT	855.20	432163.96	2471177.64
1373	148+29.74	44.45' RT	855.18	432161.53	2471177.65
1374	148+34.74	44.49' RT	855.32	432161.56	2471182.65
1375	148+34.75	43.77' RT	855.29	432162.28	2471182.65
1376	148+37.83	39.84' RT	855.10	432166.25	2471185.69
1377	148+40.92	35.90' RT	854.91	432170.22	2471188.72
1378	148+44.00	31.97' RT	854.72	432174.19	2471191.76
1379	148+45.76	29.73' RT	854.67	432176.46	2471193.49
1380	148+37.10	20.75' RT	854.72	432185.33	2471184.72
1381	148+36.71	20.96' RT	854.72	432185.11	2471184.33
1382	148+26.94	27.36' RT	854.51	432178.59	2471174.64
1384	148+38.68	17.32' RT	854.46	432188.77	2471186.25
1385	148+43.53	16.16' RT	854.43	432189.99	2471191.09

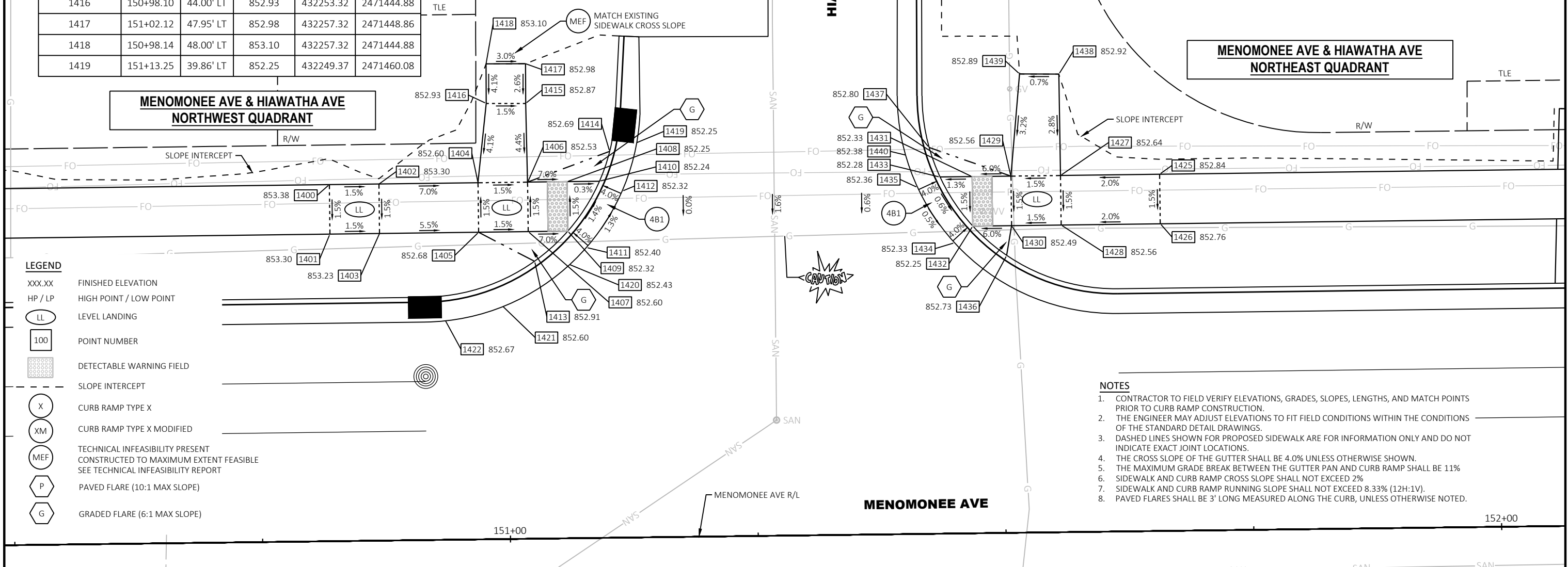
- NOTES**
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 7. SIDEWALK AND CURB RAMP RUNNING SLOPE SHALL NOT EXCEED 8.33% (12H:1V).
 8. PAVED FLARES SHALL BE 3' LONG MEASURED ALONG THE CURB, UNLESS OTHERWISE NOTED.

- LEGEND**
- XXX.XX FINISHED ELEVATION
 - HP / LP HIGH POINT / LOW POINT
 - LL LEVEL LANDING
 - 100 POINT NUMBER
 - [Pattern] DETECTABLE WARNING FIELD
 - - - SLOPE INTERCEPT
 - X CURB RAMP TYPE X
 - XM CURB RAMP TYPE X MODIFIED
 - MEF TECHNICAL INFEASIBILITY PRESENT
CONSTRUCTED TO MAXIMUM EXTENT FEASIBLE
SEE TECHNICAL INFEASIBILITY REPORT
 - P PAVED FLARE (10:1 MAX SLOPE)
 - G GRADED FLARE (6:1 MAX SLOPE)

HIAWATHA AVE NW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1400	150+82.18	36.00' LT	853.38	432245.13	2471429.07
1401	150+82.18	31.00' LT	853.30	432240.13	2471429.13
1402	150+87.18	36.00' LT	853.30	432245.19	2471434.07
1403	150+87.18	31.00' LT	853.23	432240.19	2471434.13
1404	150+97.18	36.00' LT	852.60	432245.31	2471444.06
1405	150+97.18	31.00' LT	852.68	432240.31	2471444.13
1406	151+02.18	36.00' LT	852.53	432245.38	2471449.06
1407	151+02.18	31.00' LT	852.60	432240.38	2471449.13
1408	151+06.17	36.00' LT	852.25	432245.43	2471453.06
1409	151+06.17	31.00' LT	852.32	432240.43	2471453.12
1410	151+09.42	36.00' LT	852.24	432245.47	2471456.31
1411	151+08.04	29.33' LT	852.40	432238.78	2471455.01
1412	151+11.70	34.97' LT	852.32	432244.47	2471458.60
1413	151+02.79	28.00' LT	852.91	432237.39	2471449.77
1414	151+10.49	39.00' LT	852.69	432248.48	2471457.34
1415	151+02.08	43.95' LT	852.87	432253.32	2471448.87
1416	150+98.10	44.00' LT	852.93	432253.32	2471444.88
1417	151+02.12	47.95' LT	852.98	432257.32	2471448.86
1418	150+98.14	48.00' LT	853.10	432257.32	2471444.88
1419	151+13.25	39.86' LT	852.25	432249.37	2471460.08

HIAWATHA AVE NW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1420	151+06.33	27.63' LT	852.43	432237.06	2471453.32
1421	150+99.55	23.47' LT	852.60	432232.82	2471446.59
1422	150+93.73	22.10' LT	852.67	432231.37	2471440.79

HIAWATHA AVE NE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1425	151+65.94	36.00' LT	852.84	432246.17	2471512.82
1426	151+65.94	31.00' LT	852.76	432241.17	2471512.89
1427	151+55.94	36.00' LT	852.64	432246.05	2471502.83
1428	151+55.94	31.00' LT	852.56	432241.05	2471502.89
1429	151+50.94	36.00' LT	852.56	432245.98	2471497.83
1430	151+50.94	31.00' LT	852.49	432240.99	2471497.89
1431	151+47.02	36.00' LT	852.33	432245.94	2471493.91
1432	151+47.02	31.00' LT	852.25	432240.94	2471493.97
1433	151+43.77	36.00' LT	852.28	432245.90	2471490.66
1434	151+45.16	29.33' LT	852.33	432239.25	2471492.13
1435	151+41.49	34.97' LT	852.36	432244.84	2471488.39
1436	151+50.41	28.00' LT	852.73	432237.98	2471497.39
1437	151+42.71	39.00' LT	852.80	432248.88	2471489.55
1438	151+55.91	46.23' LT	852.92	432256.27	2471502.66
1439	151+51.94	46.29' LT	852.89	432256.29	2471498.69
1440	151+40.32	38.26' LT	852.38	432248.11	2471487.18



- LEGEND**
- XXX.XX FINISHED ELEVATION
 - HP / LP HIGH POINT / LOW POINT
 - LL LEVEL LANDING
 - 100 POINT NUMBER
 - [Pattern] DETECTABLE WARNING FIELD
 - - - SLOPE INTERCEPT
 - X CURB RAMP TYPE X
 - XM CURB RAMP TYPE X MODIFIED
 - MEF TECHNICAL INFEASIBILITY PRESENT
CONSTRUCTED TO MAXIMUM EXTENT FEASIBLE
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 - P PAVED FLARE (10:1 MAX SLOPE)
 - G GRADED FLARE (6:1 MAX SLOPE)

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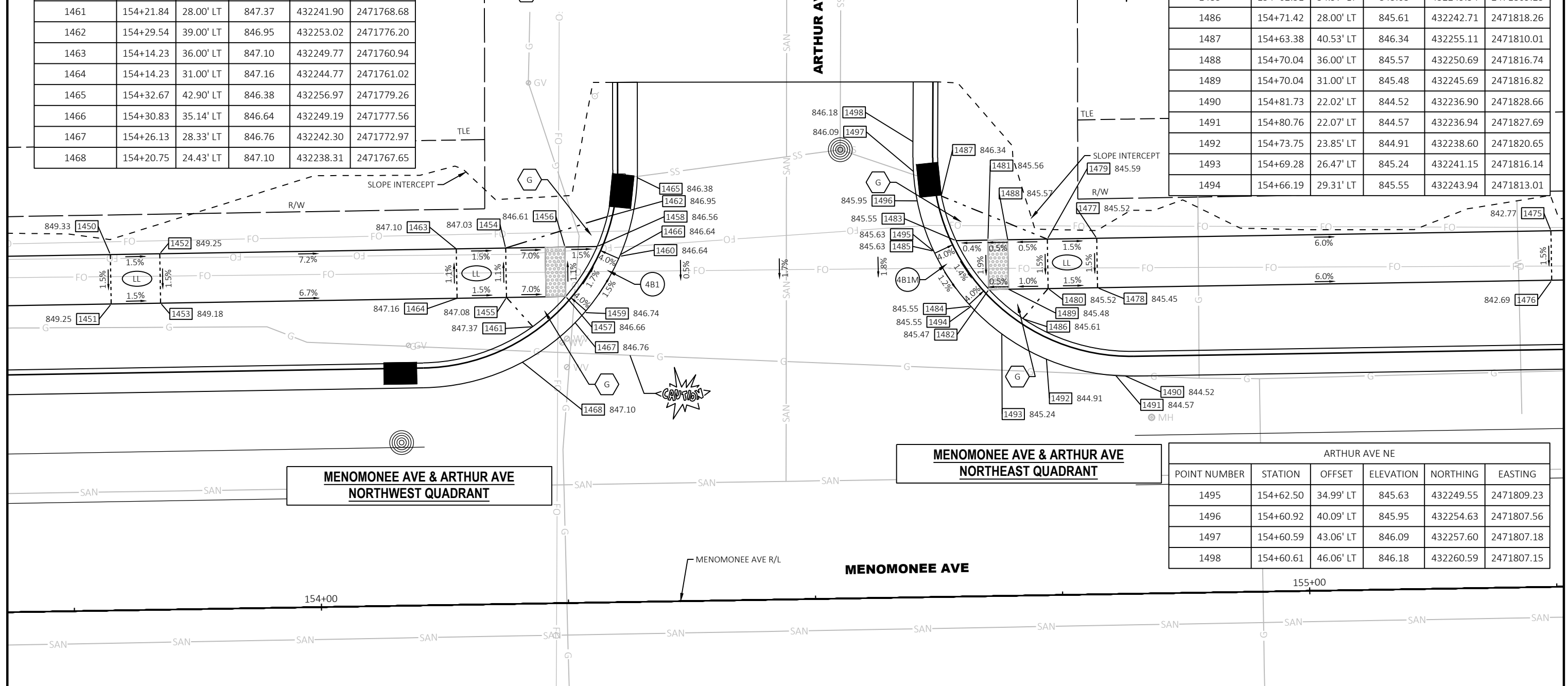
PROJECT NO: 2720-07-71	HWY: MENOMONEE AVE	COUNTY: WAUKESHA	CURB RAMP DETAILS	SHEET	E
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ARTHUR AVE NW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1450	153+79.23	36.00' LT	849.33	432249.20	2471725.95
1451	153+79.23	31.00' LT	849.25	432244.20	2471726.03
1452	153+84.23	36.00' LT	849.25	432249.28	2471730.95
1453	153+84.23	31.00' LT	849.18	432244.28	2471731.03
1454	154+19.23	36.00' LT	847.03	432249.85	2471765.94
1455	154+19.23	31.00' LT	847.08	432244.85	2471766.02
1456	154+25.23	36.00' LT	846.61	432249.95	2471771.94
1457	154+25.23	31.00' LT	846.66	432244.95	2471772.02
1458	154+28.48	36.00' LT	846.56	432250.01	2471775.19
1459	154+27.09	29.33' LT	846.74	432243.32	2471773.91
1460	154+30.76	34.97' LT	846.64	432249.02	2471777.49
1461	154+21.84	28.00' LT	847.37	432241.90	2471768.68
1462	154+29.54	39.00' LT	846.95	432253.02	2471776.20
1463	154+14.23	36.00' LT	847.10	432249.77	2471760.94
1464	154+14.23	31.00' LT	847.16	432244.77	2471761.02
1465	154+32.67	42.90' LT	846.38	432256.97	2471779.26
1466	154+30.83	35.14' LT	846.64	432249.19	2471777.56
1467	154+26.13	28.33' LT	846.76	432242.30	2471772.97
1468	154+20.75	24.43' LT	847.10	432238.31	2471767.65

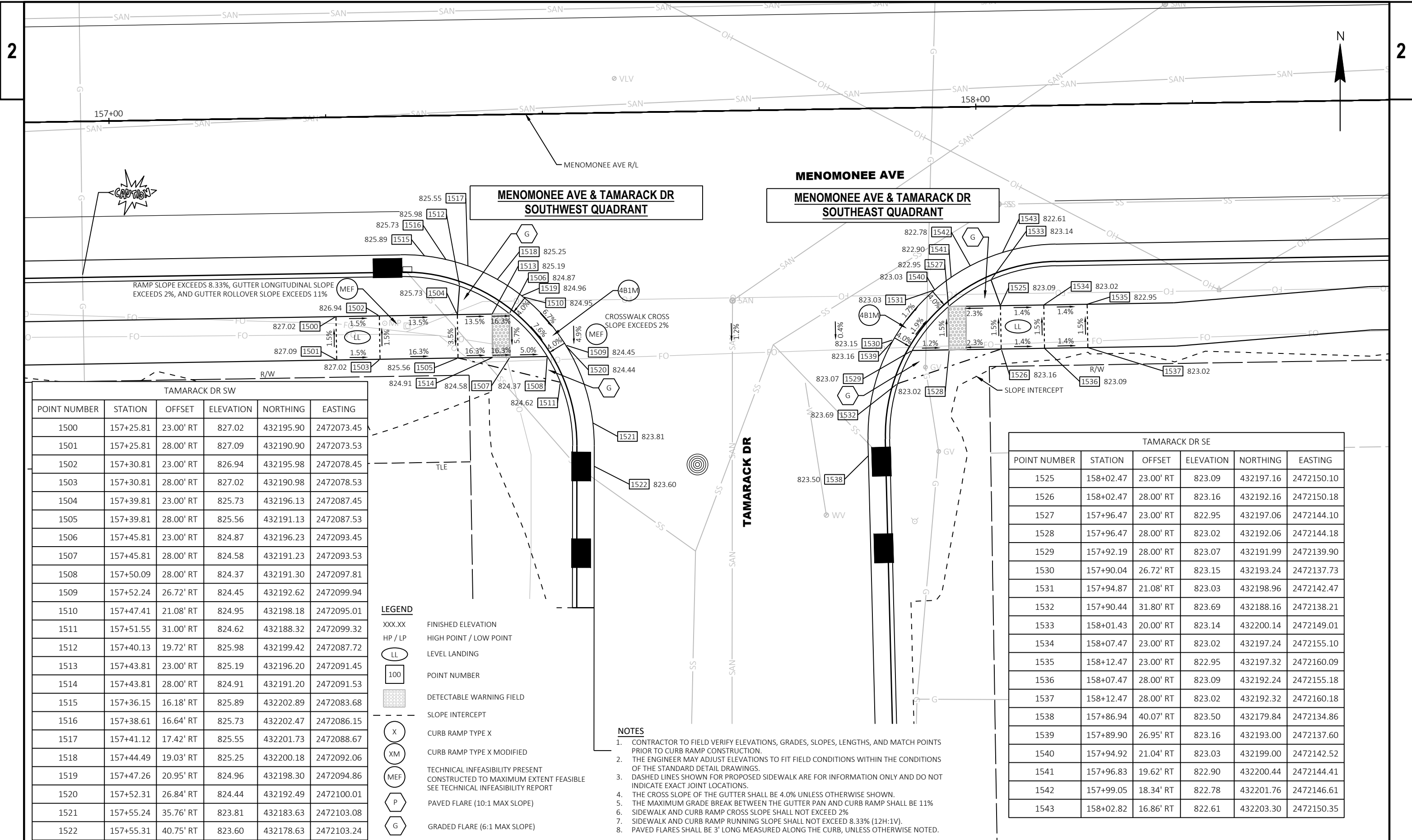
ARTHUR AVE NE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1475	155+25.00	36.00' LT	842.77	432251.59	2471871.70
1476	155+25.00	31.00' LT	842.69	432246.59	2471871.78
1477	154+79.04	36.00' LT	845.52	432250.84	2471825.74
1478	154+79.04	31.00' LT	845.45	432245.84	2471825.82
1479	154+74.04	36.00' LT	845.59	432250.75	2471820.74
1480	154+74.04	31.00' LT	845.52	432245.75	2471820.82
1481	154+68.04	36.00' LT	845.56	432250.66	2471814.74
1482	154+68.04	31.00' LT	845.47	432245.66	2471814.82
1483	154+64.79	36.00' LT	845.55	432250.60	2471811.49
1484	154+66.17	29.33' LT	845.55	432243.96	2471812.99
1485	154+62.51	34.97' LT	845.63	432249.54	2471809.23
1486	154+71.42	28.00' LT	845.61	432242.71	2471818.26
1487	154+63.38	40.53' LT	846.34	432255.11	2471810.01
1488	154+70.04	36.00' LT	845.57	432250.69	2471816.74
1489	154+70.04	31.00' LT	845.48	432245.69	2471816.82
1490	154+81.73	22.02' LT	844.52	432236.90	2471828.66
1491	154+80.76	22.07' LT	844.57	432236.94	2471827.69
1492	154+73.75	23.85' LT	844.91	432238.60	2471820.65
1493	154+69.28	26.47' LT	845.24	432241.15	2471816.14
1494	154+66.19	29.31' LT	845.55	432243.94	2471813.01

- LEGEND**
- XXX.XX FINISHED ELEVATION
 - HP / LP HIGH POINT / LOW POINT
 - LL LEVEL LANDING
 - 100 POINT NUMBER
 - [Grid Pattern] DETECTABLE WARNING FIELD
 - - - SLOPE INTERCEPT
 - X CURB RAMP TYPE X
 - XM CURB RAMP TYPE X MODIFIED
 - MEF TECHNICAL INFEASIBILITY PRESENT CONSTRUCTED TO MAXIMUM EXTENT FEASIBLE SEE TECHNICAL INFEASIBILITY REPORT
 - P PAVED FLARE (10:1 MAX SLOPE)
 - G GRADED FLARE (6:1 MAX SLOPE)

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ARTHUR AVE NE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1495	154+62.50	34.99' LT	845.63	432249.55	2471809.23
1496	154+60.92	40.09' LT	845.95	432254.63	2471807.56
1497	154+60.59	43.06' LT	846.09	432257.60	2471807.18
1498	154+60.61	46.06' LT	846.18	432260.59	2471807.15



**MENOMONEE AVE & TAMARACK DR
SOUTHWEST QUADRANT**

**MENOMONEE AVE & TAMARACK DR
SOUTHEAST QUADRANT**

TAMARACK DR SW

POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1500	157+25.81	23.00' RT	827.02	432195.90	2472073.45
1501	157+25.81	28.00' RT	827.09	432190.90	2472073.53
1502	157+30.81	23.00' RT	826.94	432195.98	2472078.45
1503	157+30.81	28.00' RT	827.02	432190.98	2472078.53
1504	157+39.81	23.00' RT	825.73	432196.13	2472087.45
1505	157+39.81	28.00' RT	825.56	432191.13	2472087.53
1506	157+45.81	23.00' RT	824.87	432196.23	2472093.45
1507	157+45.81	28.00' RT	824.58	432191.23	2472093.53
1508	157+50.09	28.00' RT	824.37	432191.30	2472097.81
1509	157+52.24	26.72' RT	824.45	432192.62	2472099.94
1510	157+47.41	21.08' RT	824.95	432198.18	2472095.01
1511	157+51.55	31.00' RT	824.62	432188.32	2472099.32
1512	157+40.13	19.72' RT	825.98	432199.42	2472087.72
1513	157+43.81	23.00' RT	825.19	432196.20	2472091.45
1514	157+43.81	28.00' RT	824.91	432191.20	2472091.53
1515	157+36.15	16.18' RT	825.89	432202.89	2472083.68
1516	157+38.61	16.64' RT	825.73	432202.47	2472086.15
1517	157+41.12	17.42' RT	825.55	432201.73	2472088.67
1518	157+44.49	19.03' RT	825.25	432200.18	2472092.06
1519	157+47.26	20.95' RT	824.96	432198.30	2472094.86
1520	157+52.31	26.84' RT	824.44	432192.49	2472100.01
1521	157+55.24	35.76' RT	823.81	432183.63	2472103.08
1522	157+55.31	40.75' RT	823.60	432178.63	2472103.24

TAMARACK DR SE

POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1525	158+02.47	23.00' RT	823.09	432197.16	2472150.10
1526	158+02.47	28.00' RT	823.16	432192.16	2472150.18
1527	157+96.47	23.00' RT	822.95	432197.06	2472144.10
1528	157+96.47	28.00' RT	823.02	432192.06	2472144.18
1529	157+92.19	28.00' RT	823.07	432191.99	2472139.90
1530	157+90.04	26.72' RT	823.15	432193.24	2472137.73
1531	157+94.87	21.08' RT	823.03	432198.96	2472142.47
1532	157+90.44	31.80' RT	823.69	432188.16	2472138.21
1533	158+01.43	20.00' RT	823.14	432200.14	2472149.01
1534	158+07.47	23.00' RT	823.02	432197.24	2472155.10
1535	158+12.47	23.00' RT	822.95	432197.32	2472160.09
1536	158+07.47	28.00' RT	823.09	432192.24	2472155.18
1537	158+12.47	28.00' RT	823.02	432192.32	2472160.18
1538	157+86.94	40.07' RT	823.50	432179.84	2472134.86
1539	157+89.90	26.95' RT	823.16	432193.00	2472137.60
1540	157+94.92	21.04' RT	823.03	432199.00	2472142.52
1541	157+96.83	19.62' RT	822.90	432200.44	2472144.41
1542	157+99.05	18.34' RT	822.78	432201.76	2472146.61
1543	158+02.82	16.86' RT	822.61	432203.30	2472150.35

- LEGEND**
- xxx.xx FINISHED ELEVATION
 - HP / LP HIGH POINT / LOW POINT
 - LL LEVEL LANDING
 - 100 POINT NUMBER
 - [Grid Pattern] DETECTABLE WARNING FIELD
 - - - SLOPE INTERCEPT
 - (X) CURB RAMP TYPE X
 - (XM) CURB RAMP TYPE X MODIFIED
 - (MEF) TECHNICAL INFEASIBILITY PRESENT
CONSTRUCTED TO MAXIMUM EXTENT FEASIBLE
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 - (G) GRADED FLARE (6:1 MAX SLOPE)

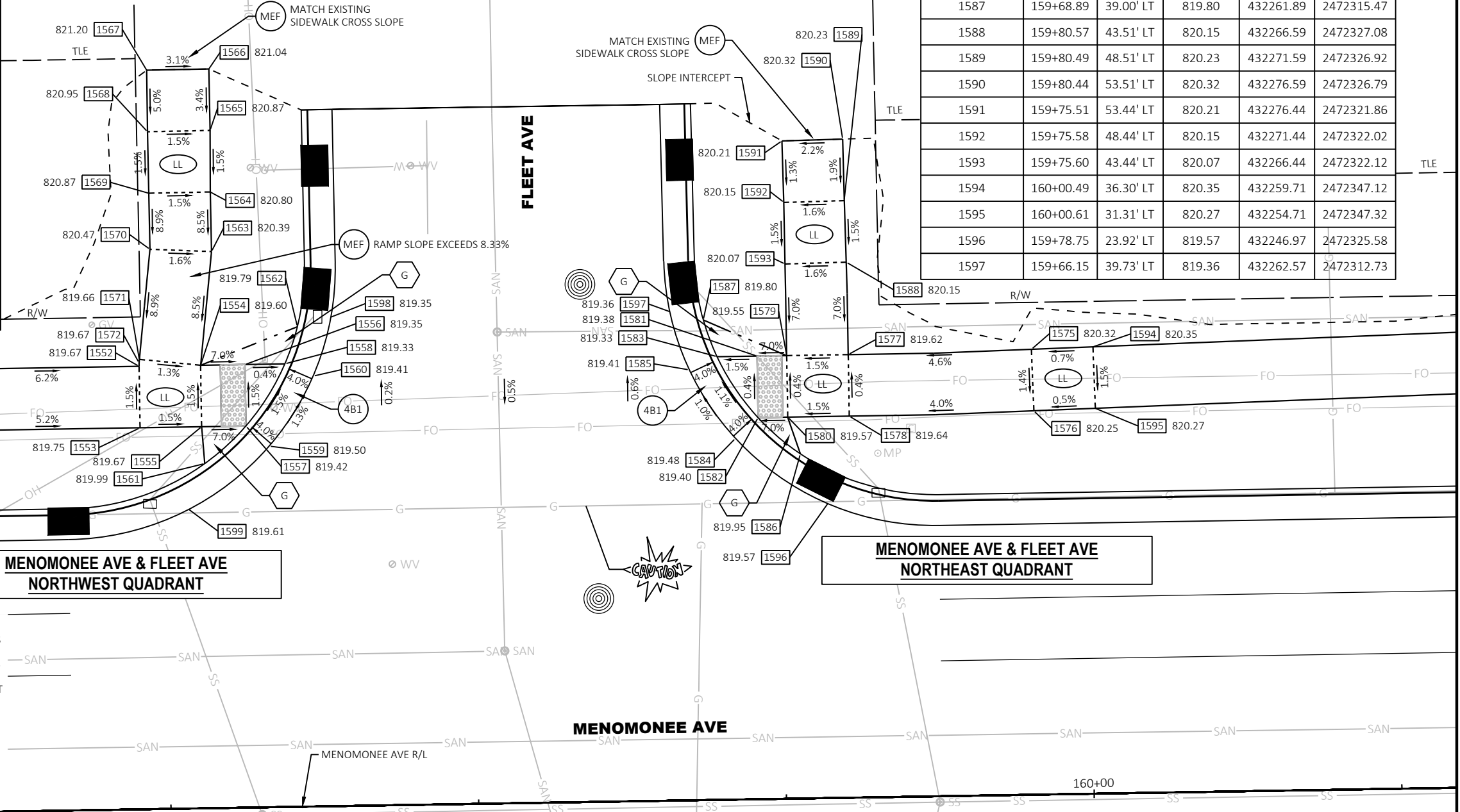
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FLEET AVE NW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1550	159+08.00	36.00' LT	820.60	432257.89	2472254.64
1551	159+08.00	31.00' LT	820.53	432252.89	2472254.72
1552	159+23.00	36.00' LT	819.67	432258.13	2472269.64
1553	159+23.00	31.00' LT	819.75	432253.13	2472269.72
1554	159+28.00	36.00' LT	819.60	432258.21	2472274.64
1555	159+28.00	31.00' LT	819.67	432253.21	2472274.72
1556	159+31.60	36.00' LT	819.35	432258.27	2472278.24
1557	159+31.60	31.00' LT	819.42	432253.27	2472278.32
1558	159+34.85	36.00' LT	819.33	432258.33	2472281.49
1559	159+33.46	29.33' LT	819.50	432251.64	2472280.21
1560	159+37.13	34.97' LT	819.41	432257.34	2472283.79
1561	159+28.21	28.00' LT	819.99	432250.22	2472274.98
1562	159+35.91	39.00' LT	819.79	432261.34	2472282.50
1563	159+29.03	45.22' LT	820.39	432267.45	2472275.53
1564	159+29.01	50.06' LT	820.80	432272.29	2472275.43
1565	159+29.08	55.06' LT	820.87	432277.29	2472275.41
1566	159+29.06	60.06' LT	821.04	432282.29	2472275.31
1567	159+24.01	60.04' LT	821.20	432282.19	2472270.26
1568	159+23.99	55.04' LT	820.95	432277.19	2472270.32
1569	159+24.01	50.04' LT	820.87	432272.19	2472270.43
1570	159+24.03	45.49' LT	820.47	432267.64	2472270.52
1571	159+23.03	36.56' LT	819.66	432258.69	2472269.66
1572	159+23.00	36.28' LT	819.67	432258.41	2472269.63
1573	159+03.00	36.00' LT	820.67	432257.80	2472249.64
1574	159+03.00	31.00' LT	820.60	432252.80	2472249.72

FLEET AVE NW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1598	159+38.64	39.73' LT	819.35	432262.12	2472285.22
1599	159+26.85	24.30' LT	819.61	432246.49	2472273.68

FLEET AVE NE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1575	159+95.49	36.19' LT	820.32	432259.51	2472342.12
1576	159+95.61	31.19' LT	820.25	432254.51	2472342.32
1577	159+80.61	36.00' LT	819.62	432259.08	2472327.25
1578	159+80.61	31.00' LT	819.64	432254.08	2472327.33
1579	159+75.61	36.00' LT	819.55	432259.00	2472322.25
1580	159+75.61	31.00' LT	819.57	432254.00	2472322.33
1581	159+73.20	36.00' LT	819.38	432258.96	2472319.84
1582	159+73.20	31.00' LT	819.40	432253.96	2472319.92
1583	159+69.95	36.00' LT	819.33	432258.90	2472316.59
1584	159+71.34	29.33' LT	819.48	432252.26	2472318.08
1585	159+67.67	34.97' LT	819.41	432257.84	2472314.33
1586	159+76.59	28.00' LT	819.95	432251.01	2472323.36
1587	159+68.89	39.00' LT	819.80	432261.89	2472315.47
1588	159+80.57	43.51' LT	820.15	432266.59	2472327.08
1589	159+80.49	48.51' LT	820.23	432271.59	2472326.92
1590	159+80.44	53.51' LT	820.32	432276.59	2472326.79
1591	159+75.51	53.44' LT	820.21	432276.44	2472321.86
1592	159+75.58	48.44' LT	820.15	432271.44	2472322.02
1593	159+75.60	43.44' LT	820.07	432266.44	2472322.12
1594	160+00.49	36.30' LT	820.35	432259.71	2472347.12
1595	160+00.61	31.31' LT	820.27	432254.71	2472347.32
1596	159+78.75	23.92' LT	819.57	432246.97	2472325.58
1597	159+66.15	39.73' LT	819.36	432262.57	2472312.73











- LEGEND**
- XXX.XX FINISHED ELEVATION
 - HP / LP HIGH POINT / LOW POINT
 - LL LEVEL LANDING
 - 100 POINT NUMBER
 - [Pattern] DETECTABLE WARNING FIELD
 - - - SLOPE INTERCEPT
 - X CURB RAMP TYPE X
 - XM CURB RAMP TYPE X MODIFIED
 - MEF TECHNICAL INFEASIBILITY PRESENT
CONSTRUCTED TO MAXIMUM EXTENT FEASIBLE
SEE TECHNICAL INFEASIBILITY REPORT
 - P PAVED FLARE (10:1 MAX SLOPE)
 - G GRADED FLARE (6:1 MAX SLOPE)

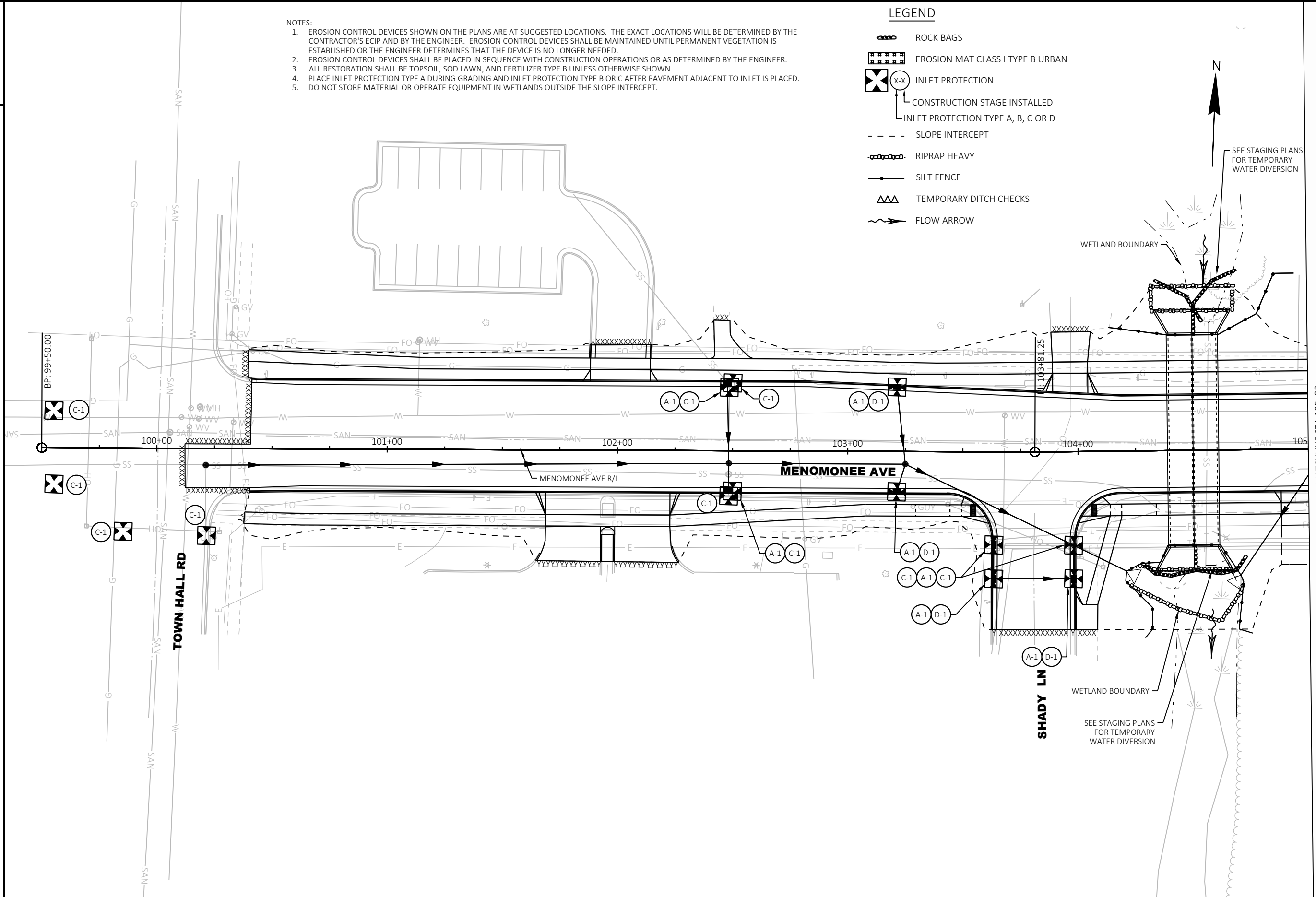


- NOTES**
- CONTRACTOR TO FIELD VERIFY ELEVATIONS, GRADES, SLOPES, LENGTHS, AND MATCH POINTS PRIOR TO CURB RAMP CONSTRUCTION.
 - THE ENGINEER MAY ADJUST ELEVATIONS TO FIT FIELD CONDITIONS WITHIN THE CONDITIONS OF THE STANDARD DETAIL DRAWINGS.
 - DASHED LINES SHOWN FOR PROPOSED SIDEWALK ARE FOR INFORMATION ONLY AND DO NOT INDICATE EXACT JOINT LOCATIONS.
 - THE CROSS SLOPE OF THE GUTTER SHALL BE 4.0% UNLESS OTHERWISE SHOWN.
 - THE MAXIMUM GRADE BREAK BETWEEN THE GUTTER PAN AND CURB RAMP SHALL BE 11%.
 - SIDEWALK AND CURB RAMP CROSS SLOPE SHALL NOT EXCEED 2%.
 - SIDEWALK AND CURB RAMP RUNNING SLOPE SHALL NOT EXCEED 8.33% (12H:1V).
 - PAVED FLARES SHALL BE 3' LONG MEASURED ALONG THE CURB, UNLESS OTHERWISE NOTED.











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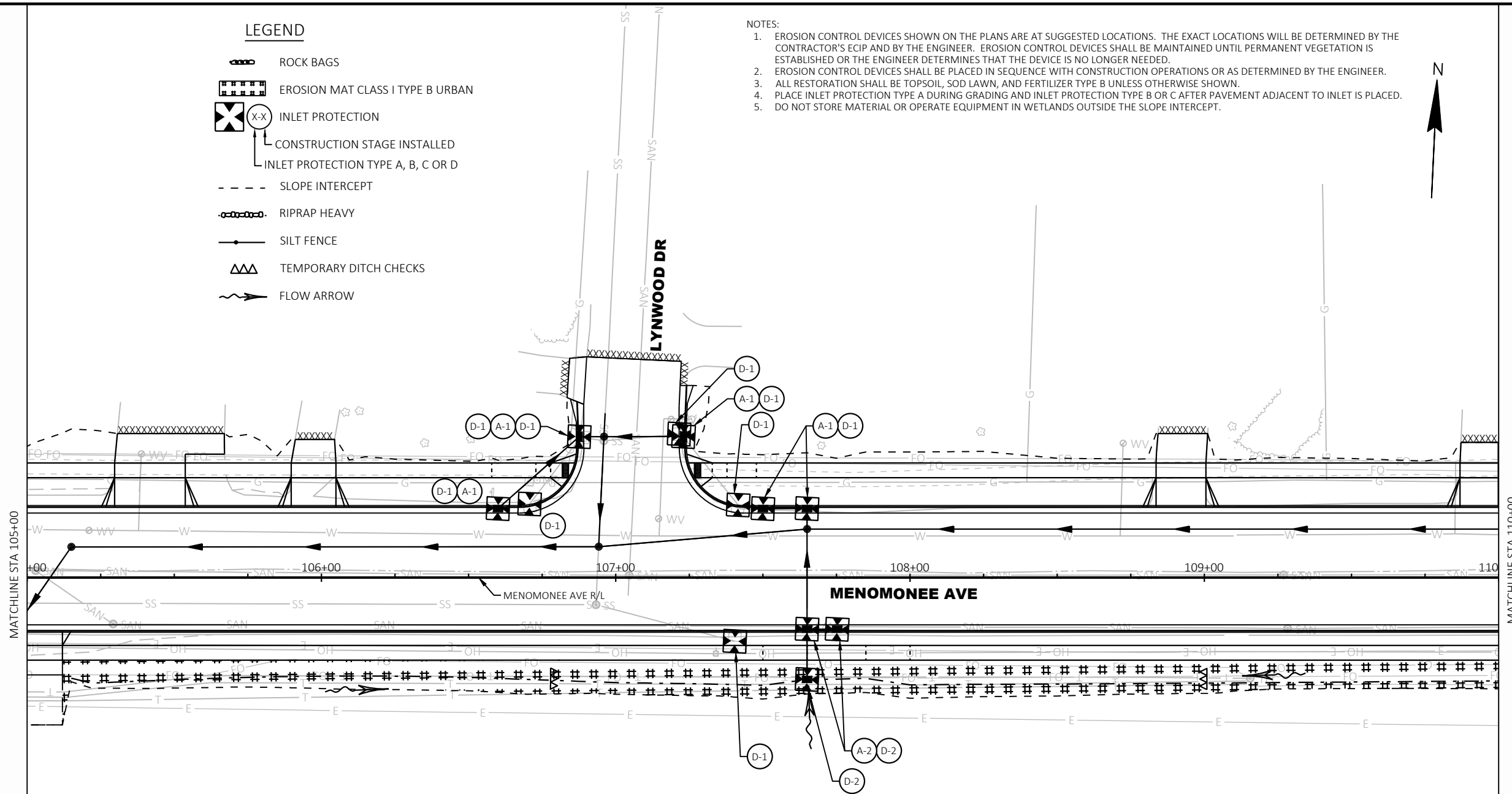


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








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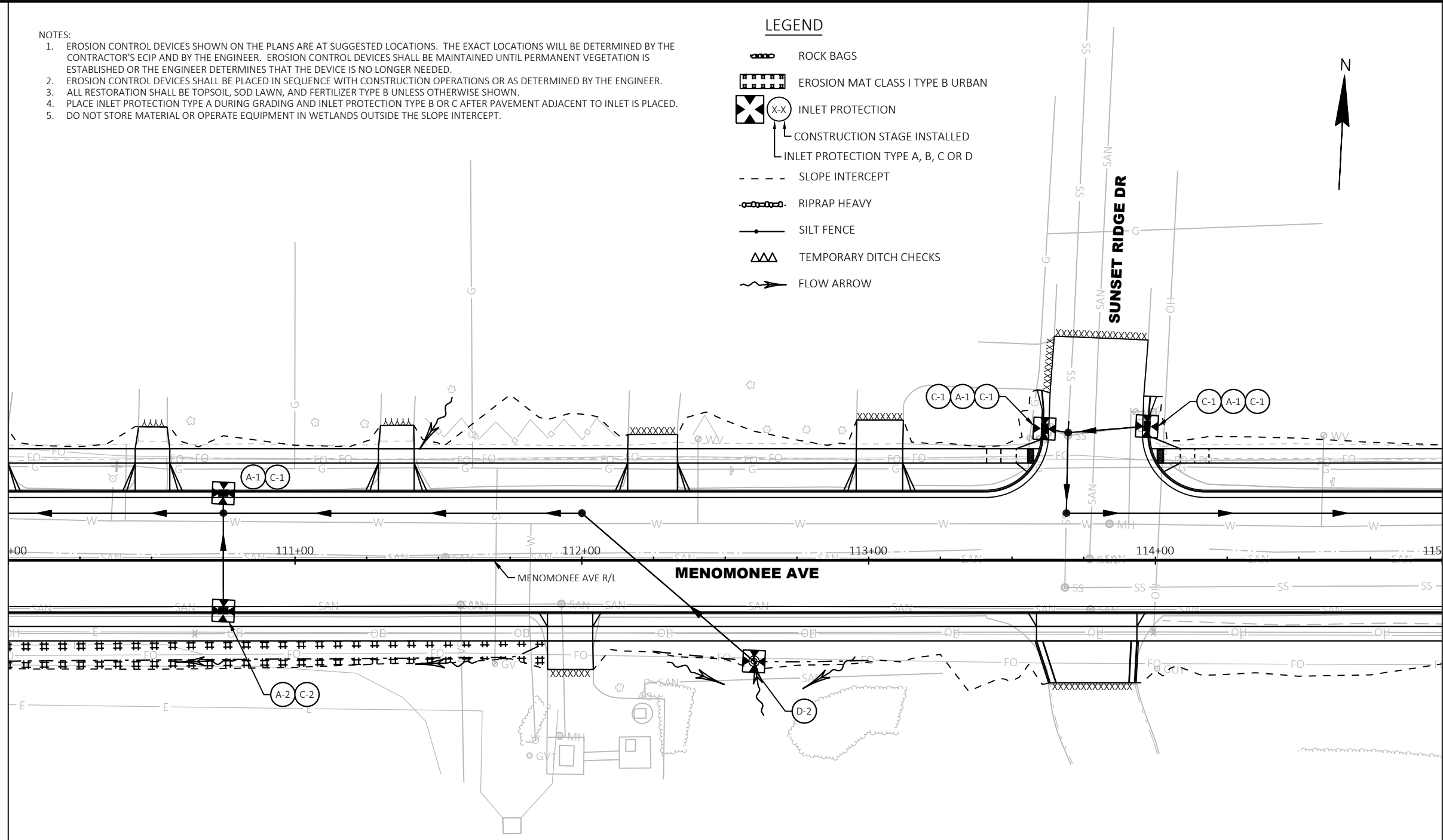
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MATCHLINE STA 110+00


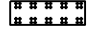




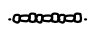
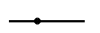


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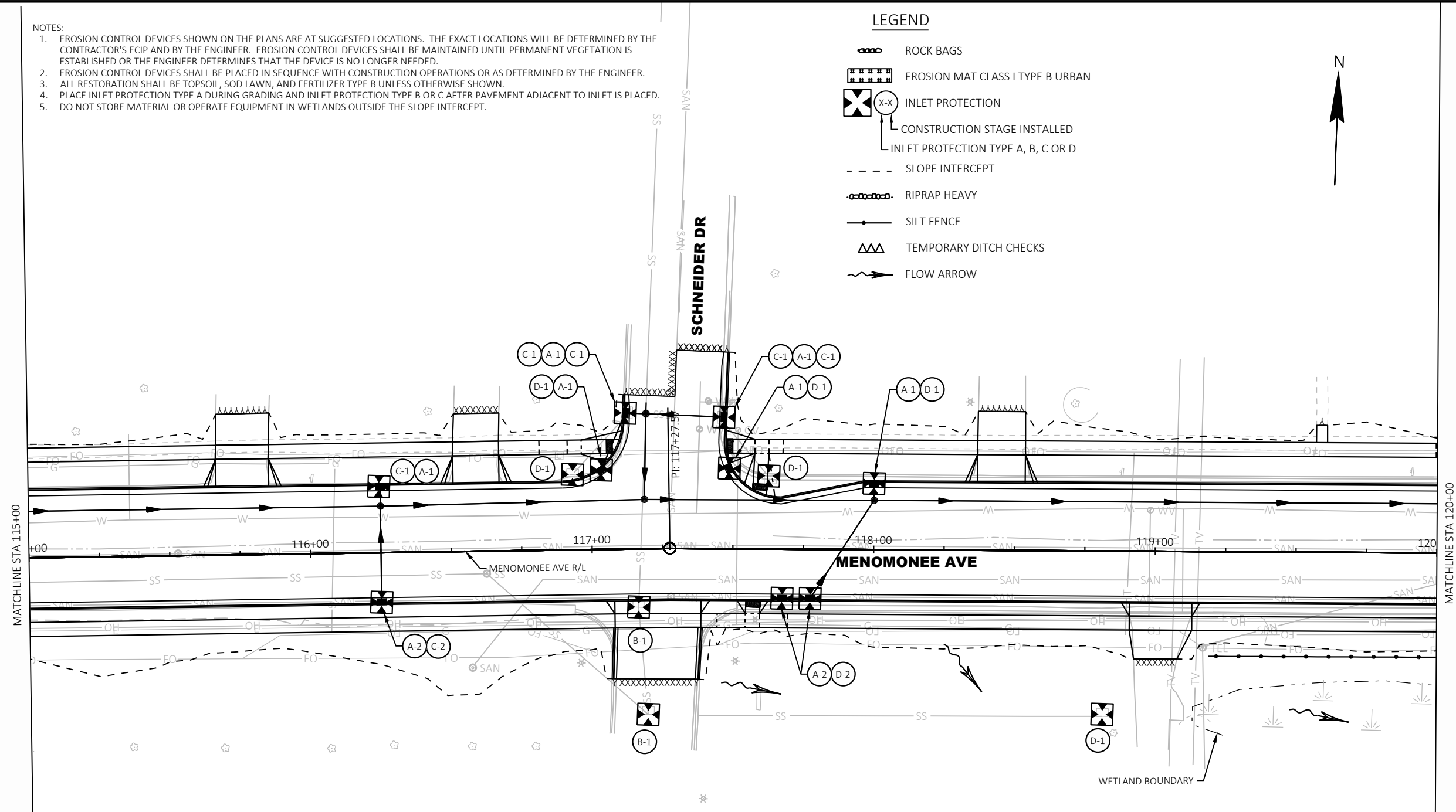


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








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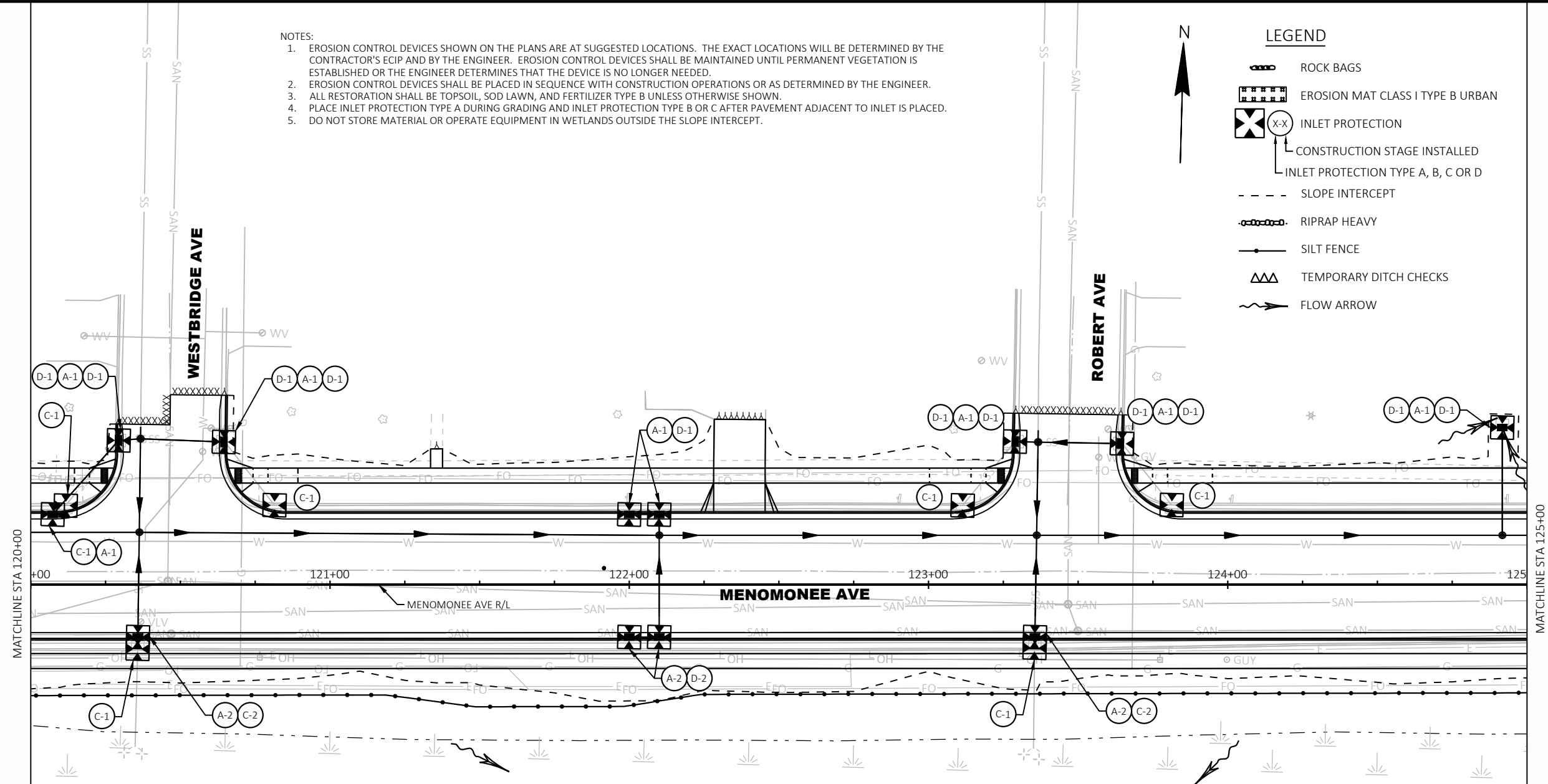
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




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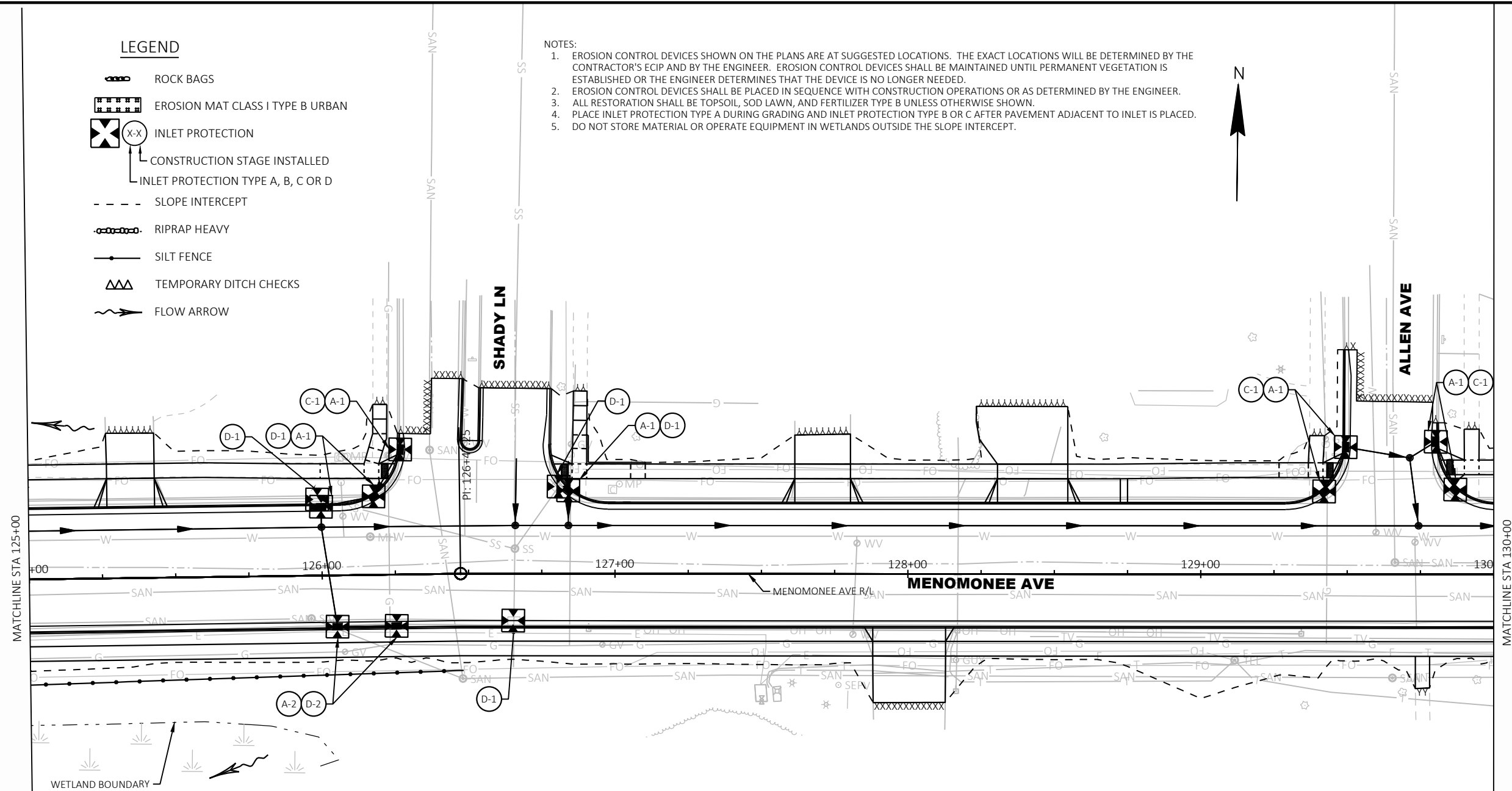
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








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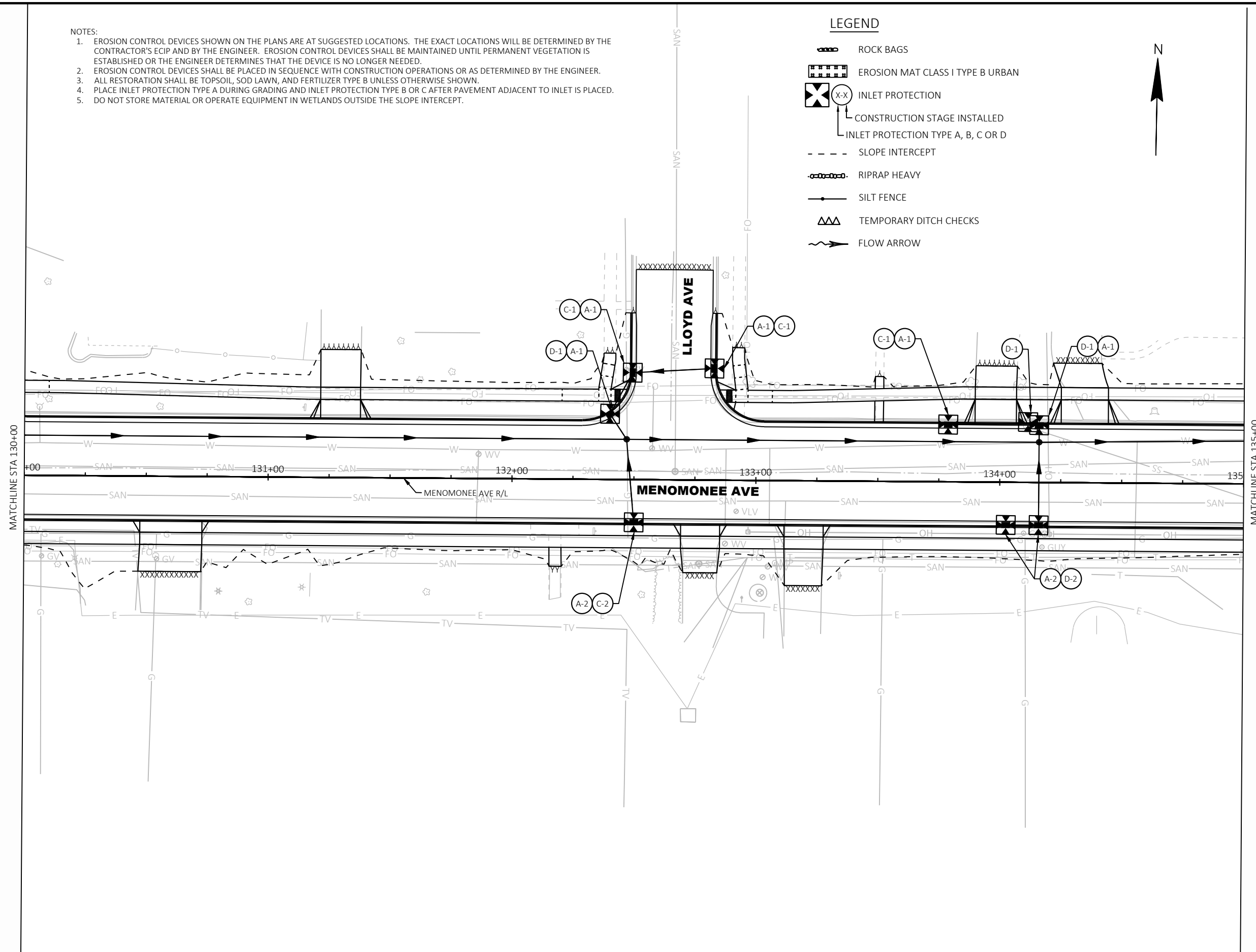


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








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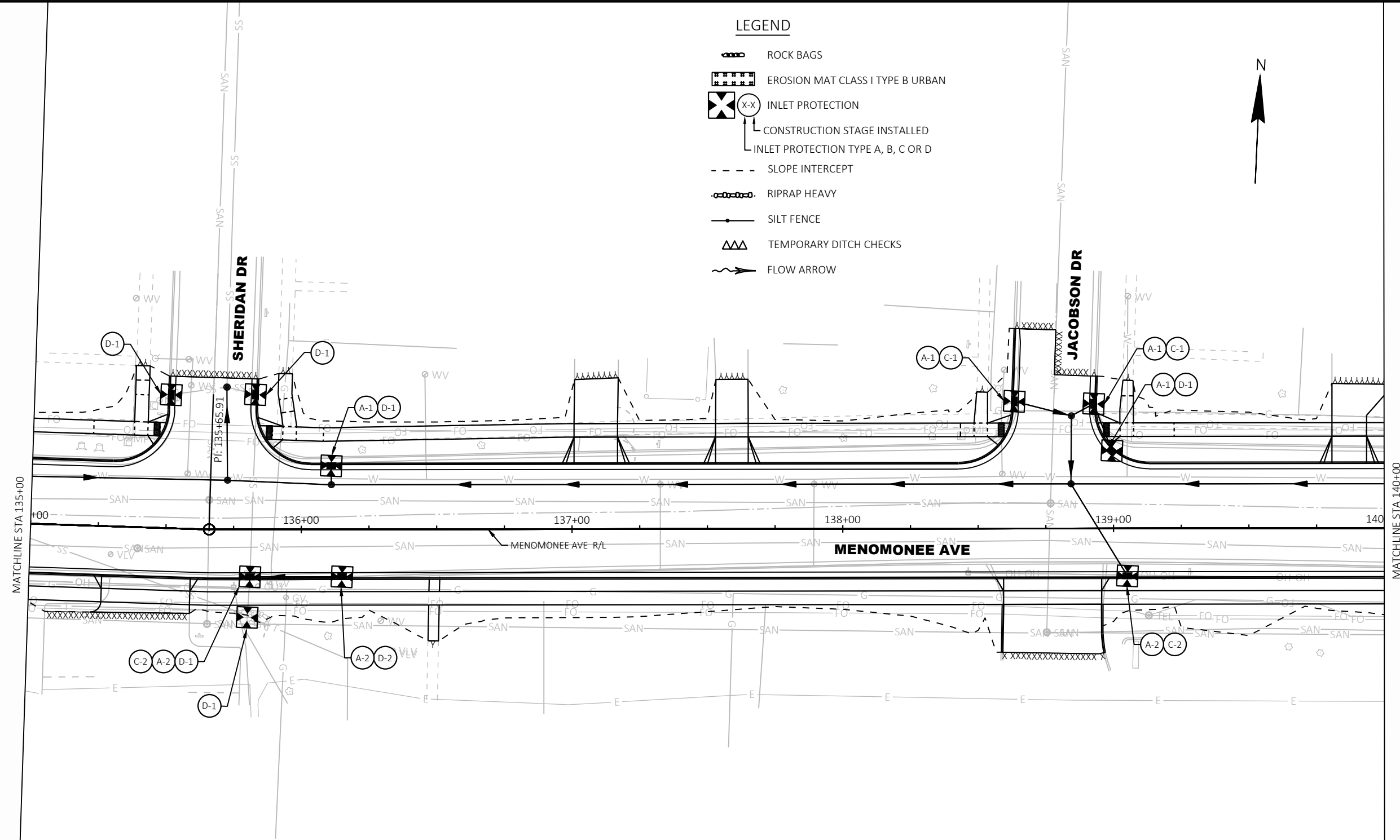
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



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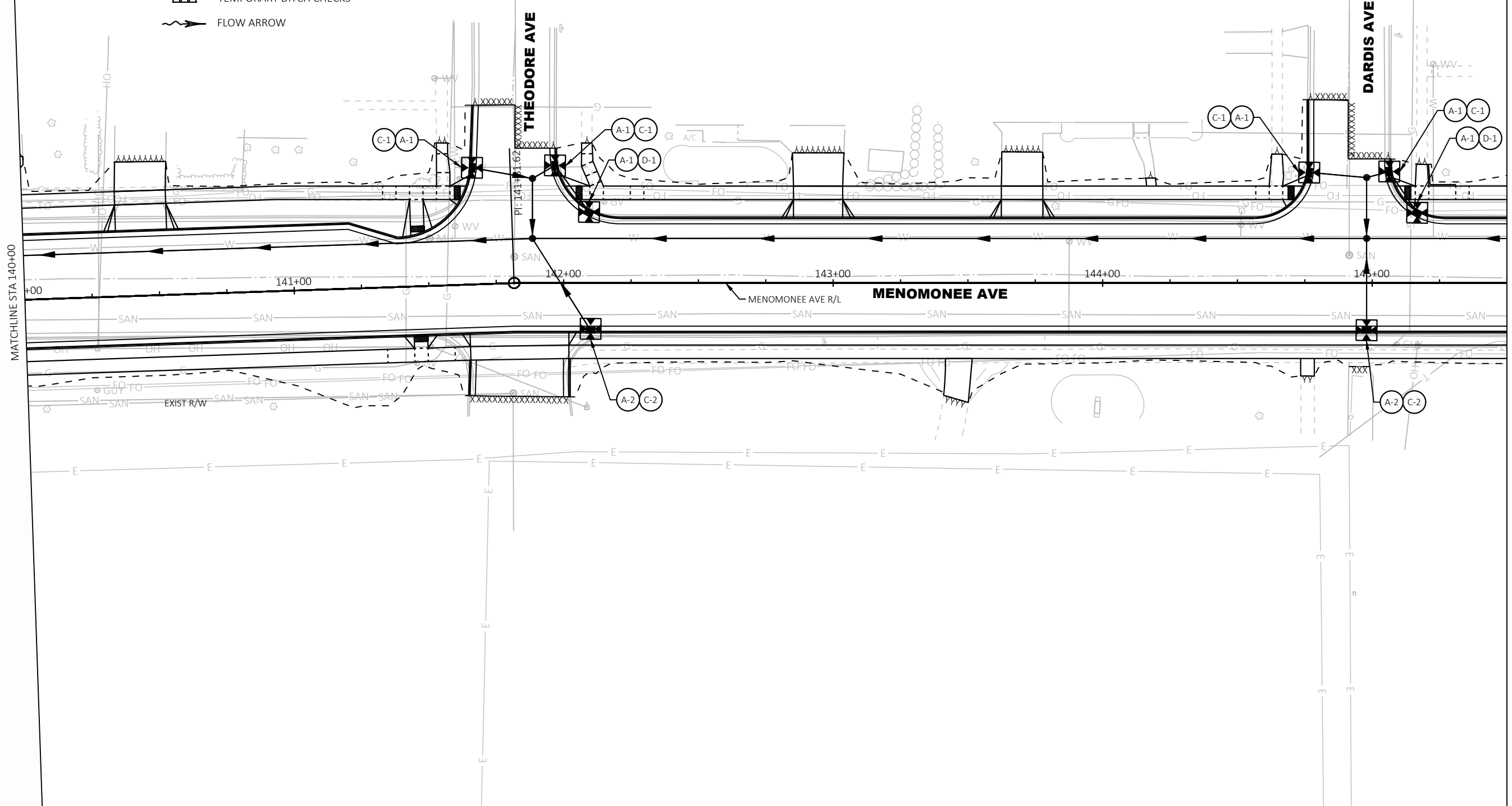
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PROJECT NO: 2720-07-71

HWY: MENOMONEE AVE

COUNTY: WAUKESHA











EROSION CONTROL

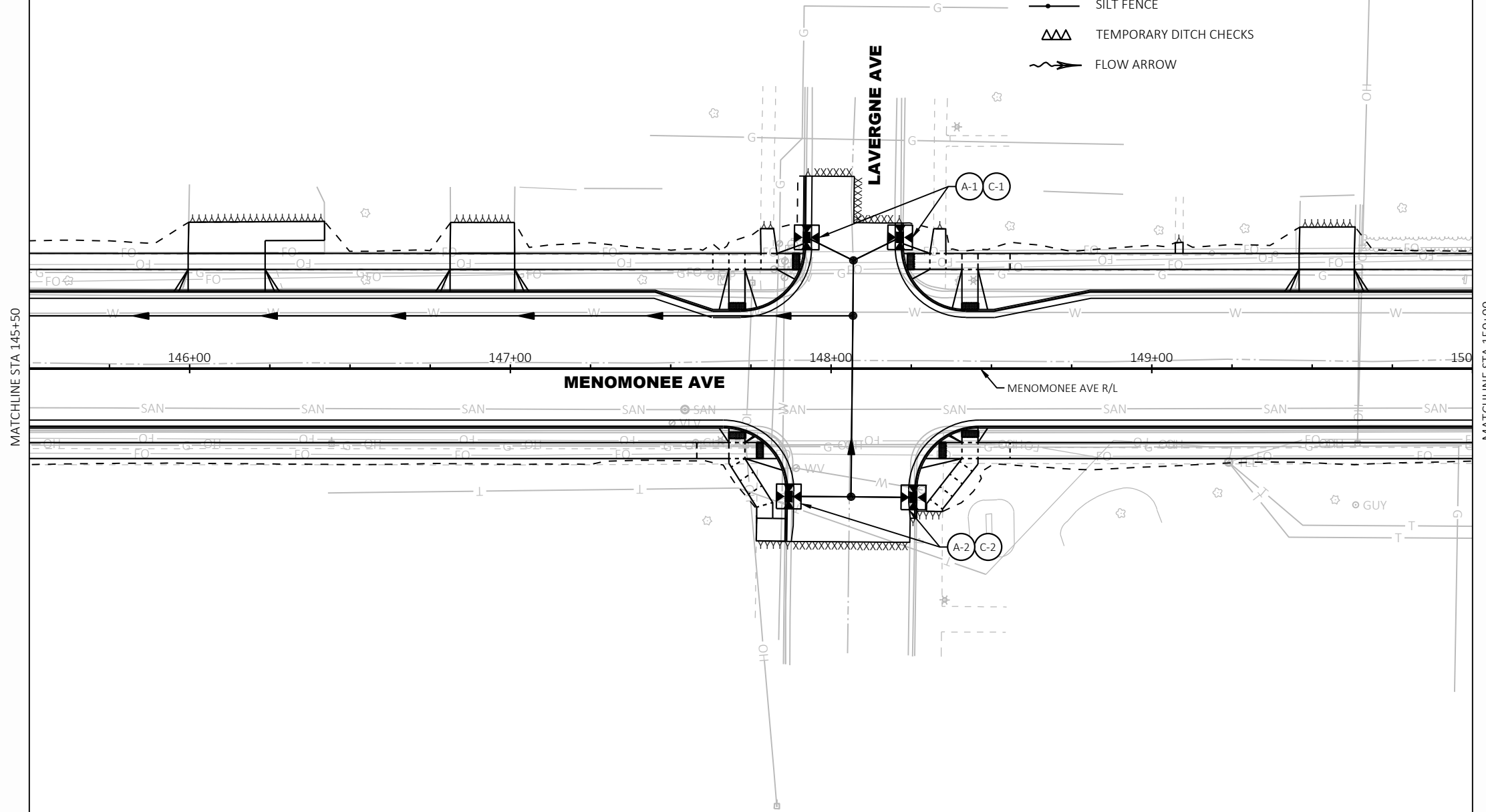
SHEET

E











- NOTES:
1. EROSION CONTROL DEVICES SHOWN ON THE PLANS ARE AT SUGGESTED LOCATIONS. THE EXACT LOCATIONS WILL BE DETERMINED BY THE CONTRACTOR'S EICP AND BY THE ENGINEER. EROSION CONTROL DEVICES SHALL BE MAINTAINED UNTIL PERMANENT VEGETATION IS ESTABLISHED OR THE ENGINEER DETERMINES THAT THE DEVICE IS NO LONGER NEEDED.
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 5. DO NOT STORE MATERIAL OR OPERATE EQUIPMENT IN WETLANDS OUTSIDE THE SLOPE INTERCEPT.

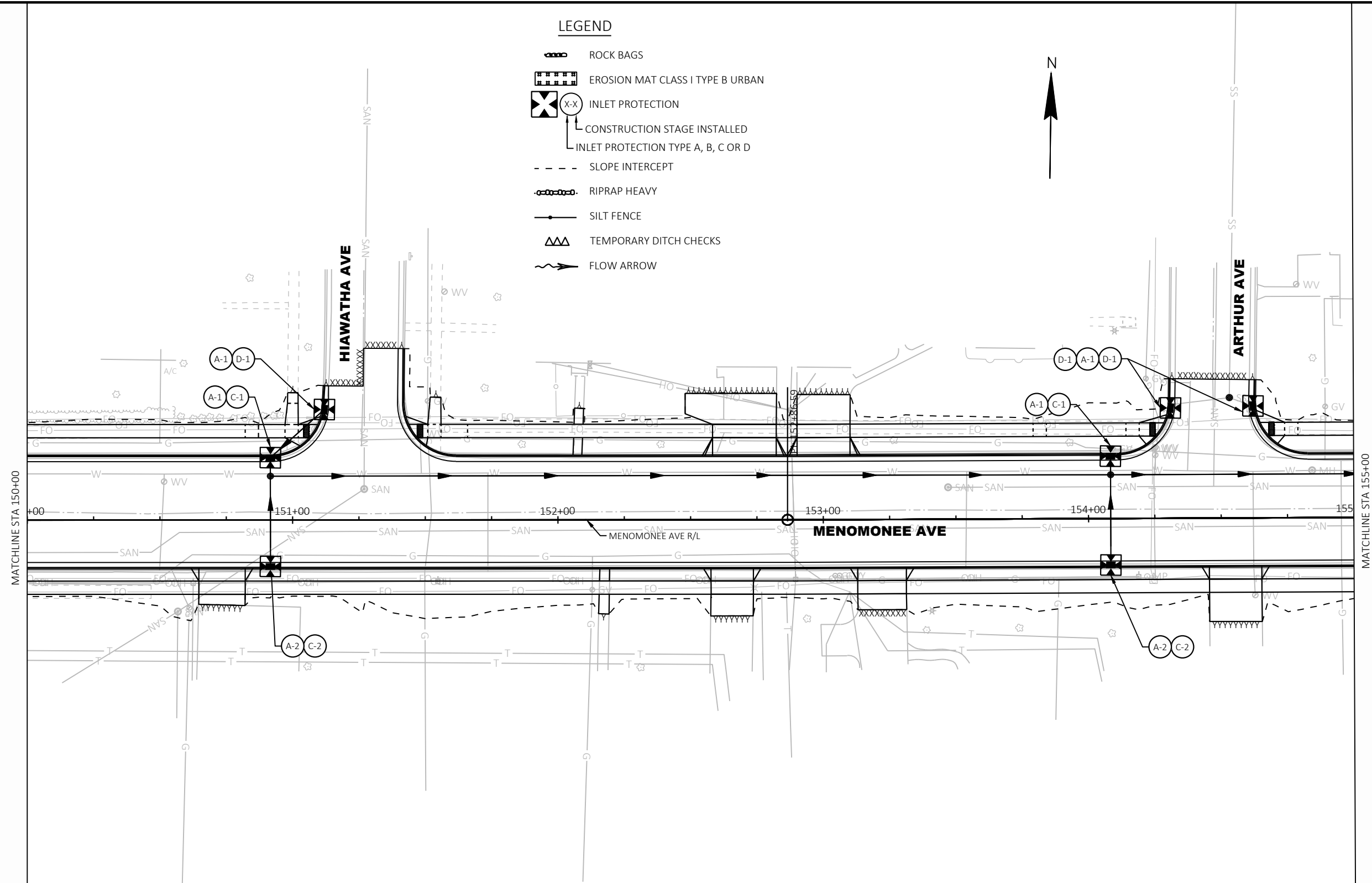
LEGEND

-  ROCK BAGS
-  EROSION MAT CLASS I TYPE B URBAN
-  INLET PROTECTION
-  CONSTRUCTION STAGE INSTALLED
-  INLET PROTECTION TYPE A, B, C OR D
-  SLOPE INTERCEPT
-  RIPRAP HEAVY
-  SILT FENCE
-  TEMPORARY DITCH CHECKS
-  FLOW ARROW



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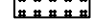




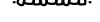





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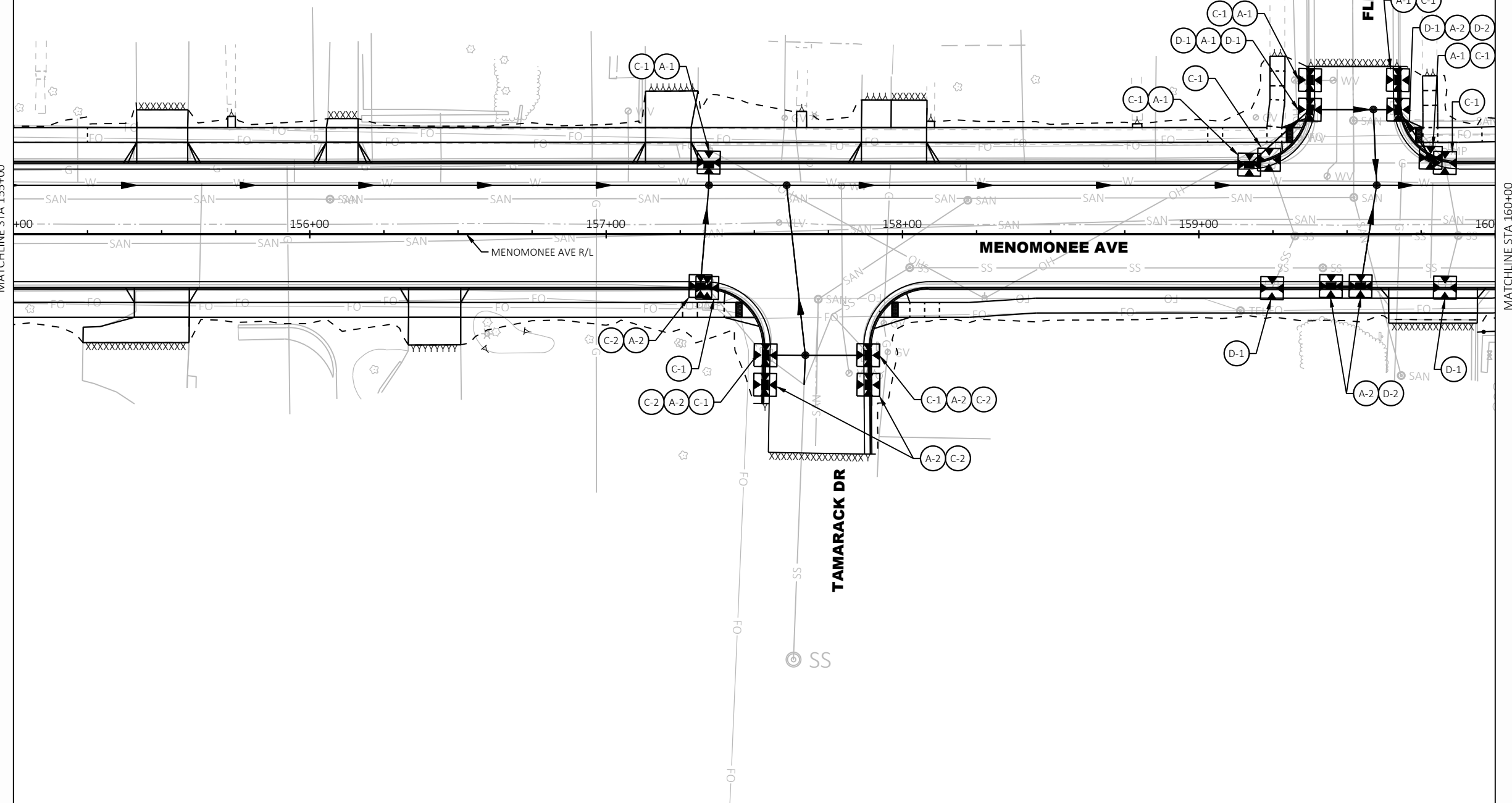
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MATCHLINE STA 155+00













MATCHLINE STA 160+00

NOTES:

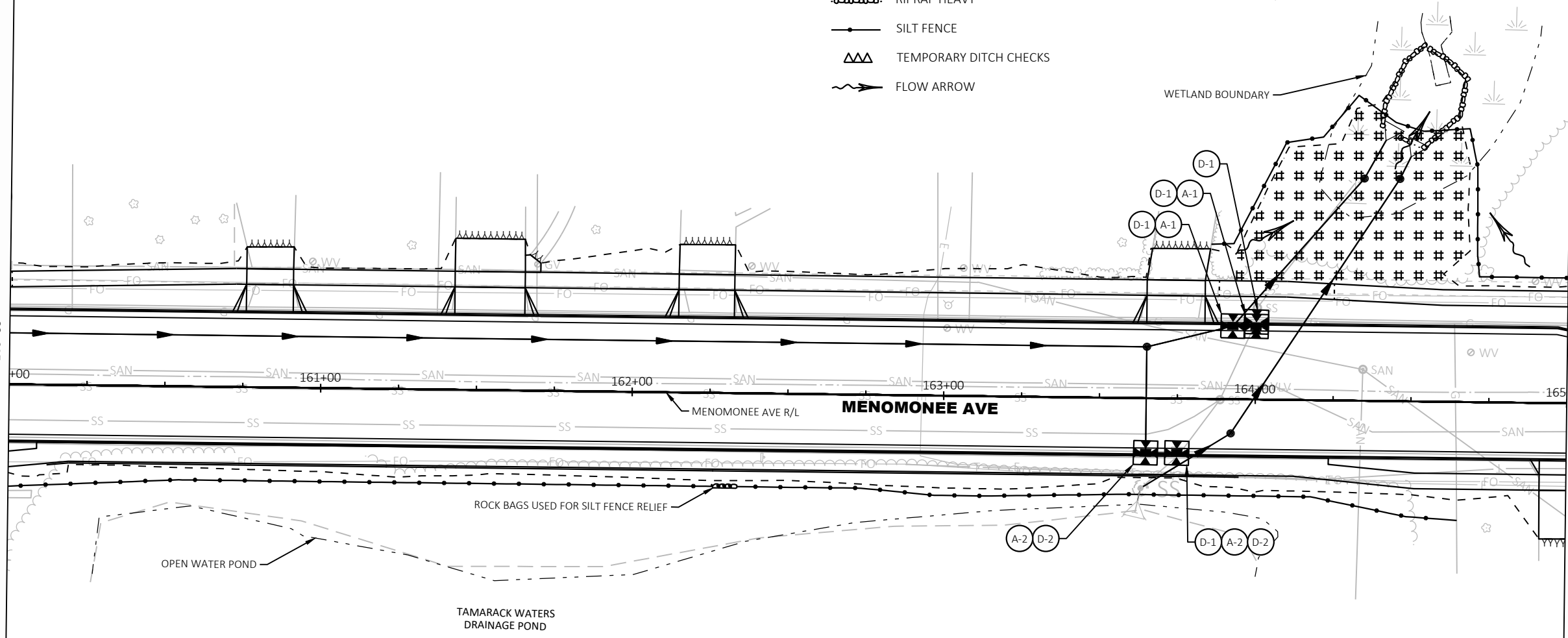
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MATCHLINE STA 160+00













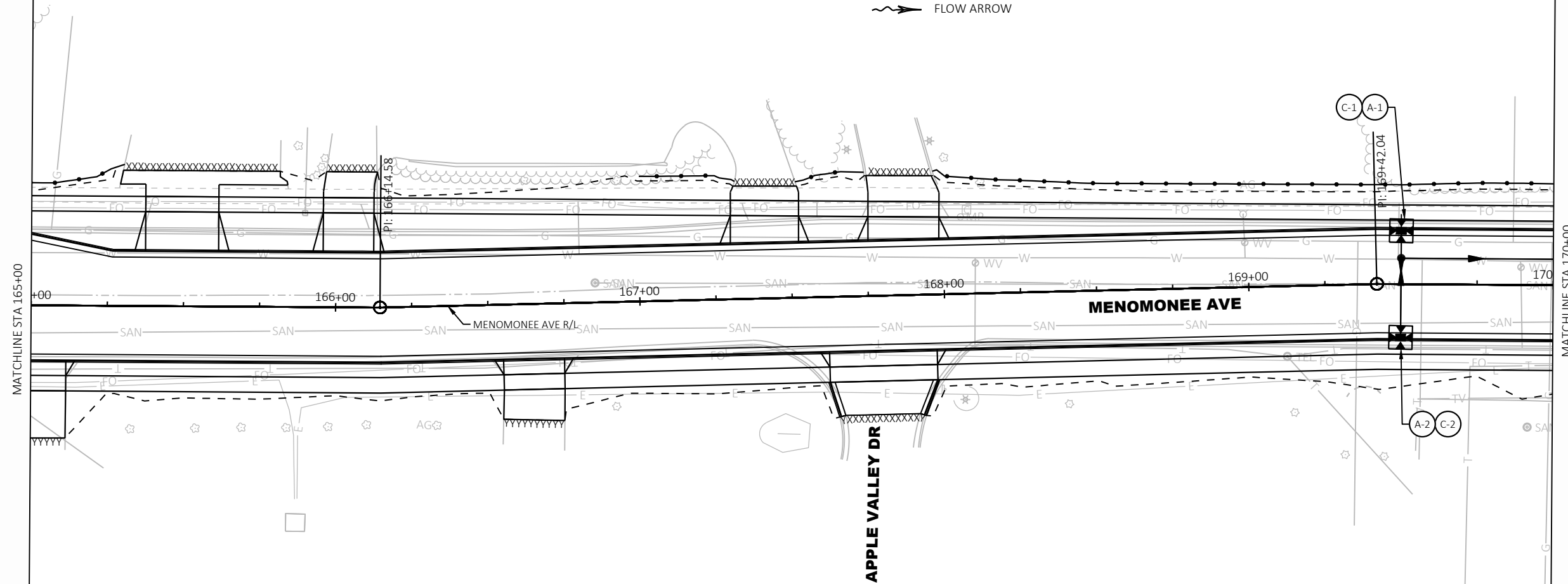
MATCHLINE STA 165+00

NOTES:











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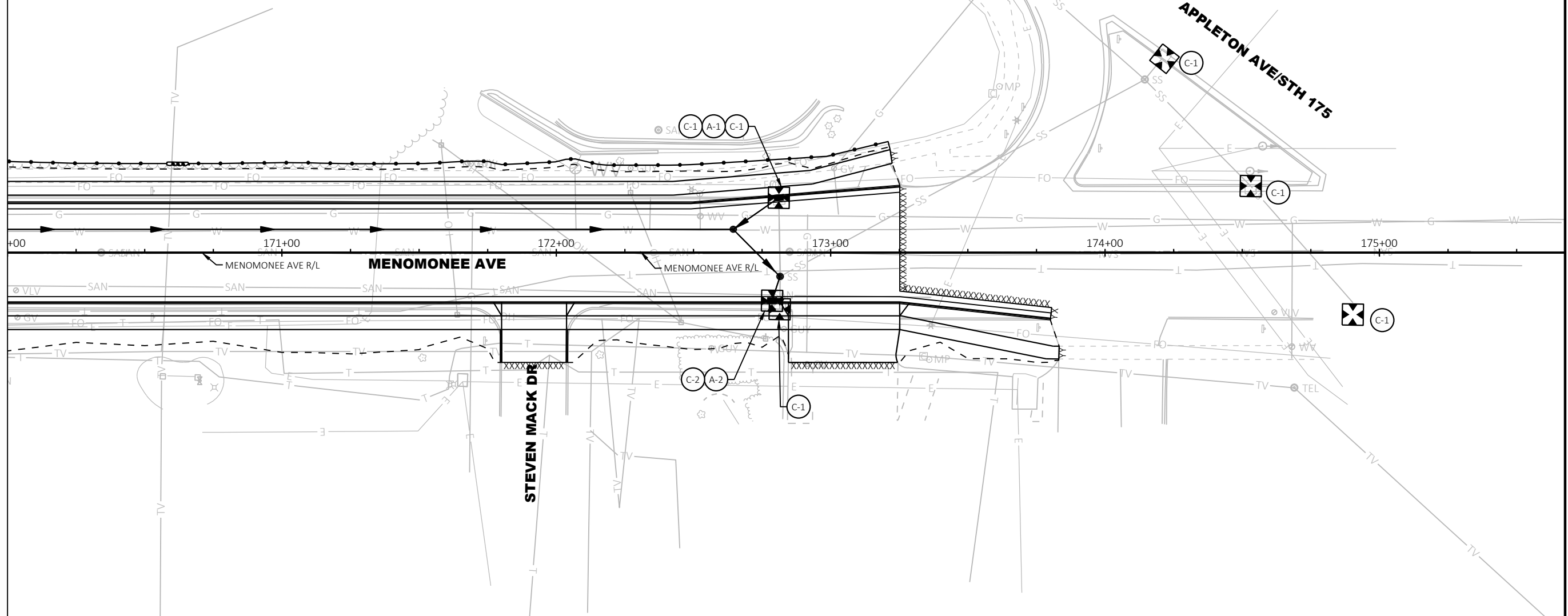


LEGEND

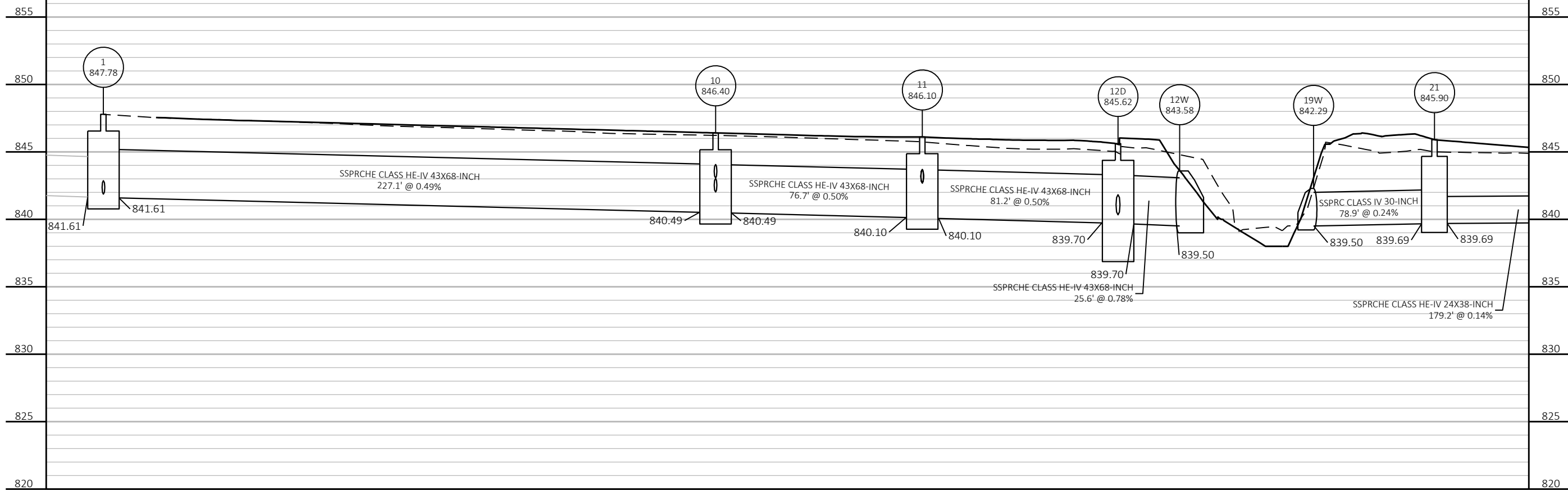
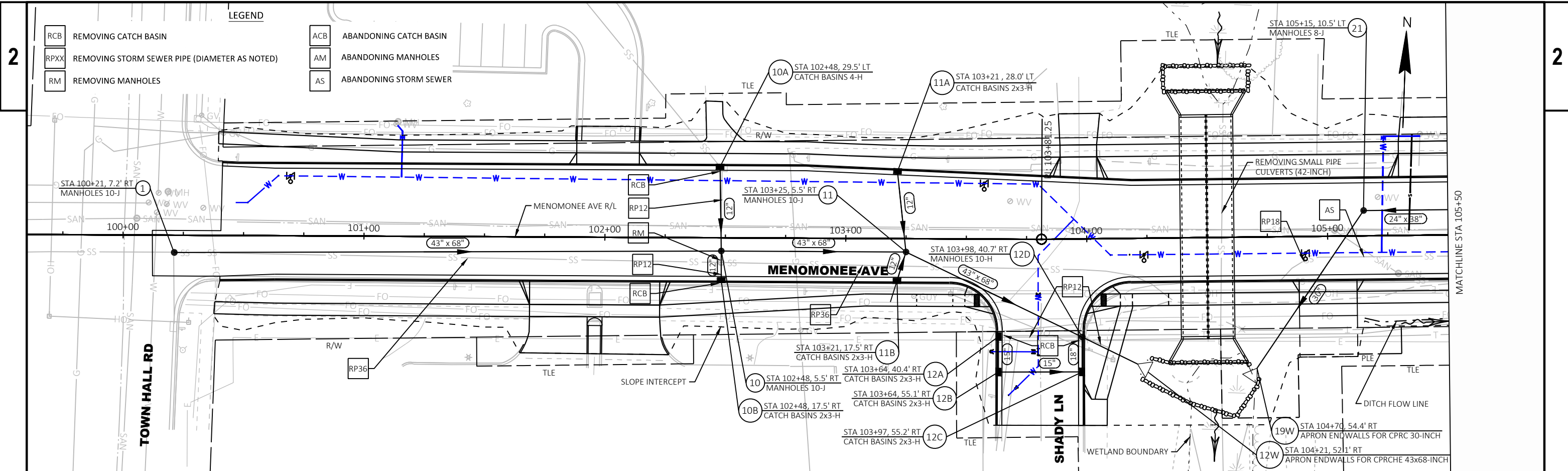
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MATCHLINE STA. 170+00



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PROJECT NO: 2720-07-71

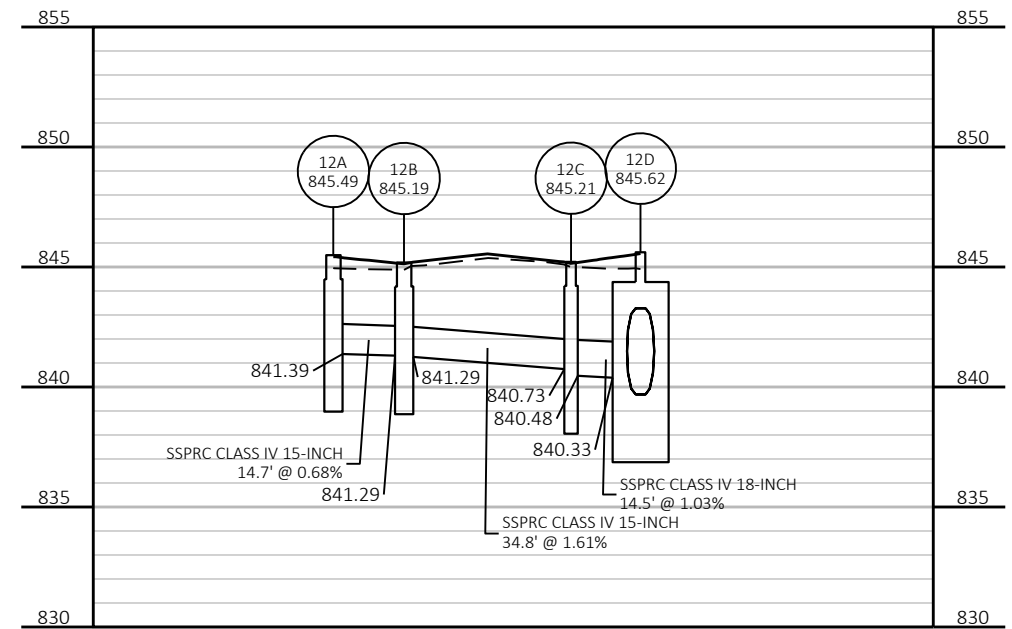
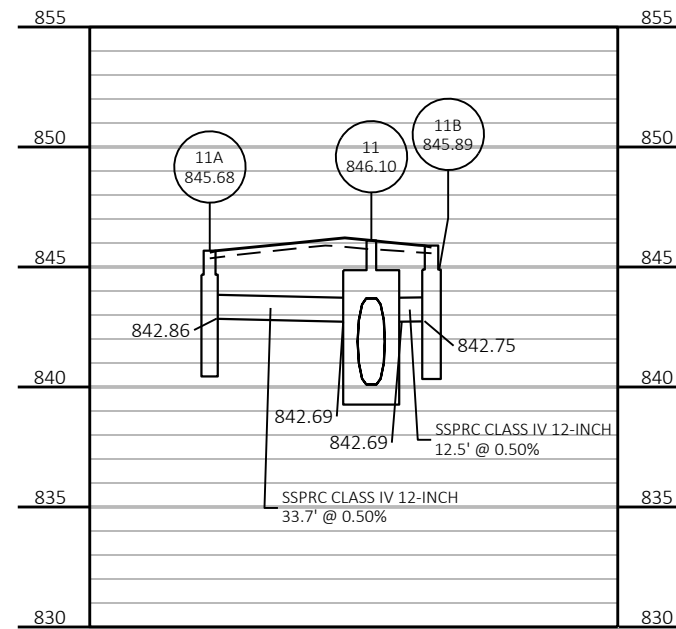
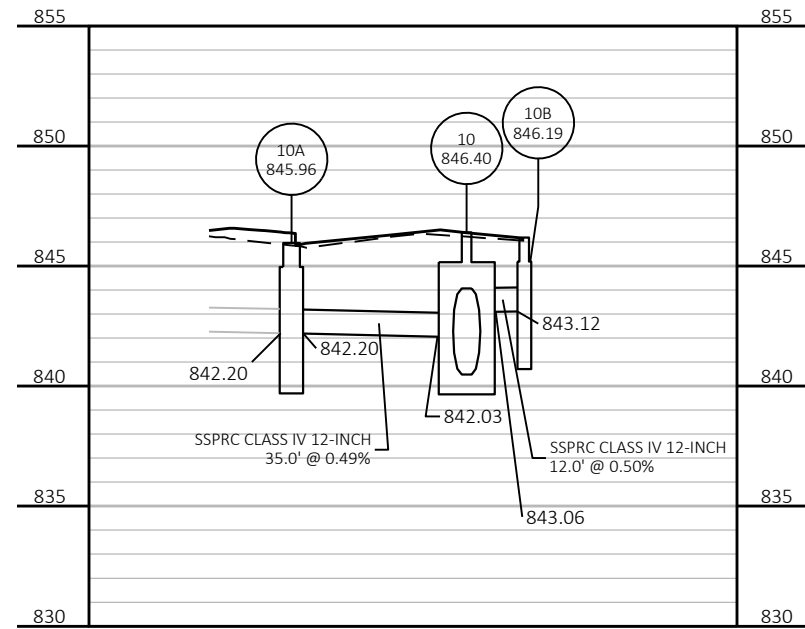
HWY: MENOMONEE AVE

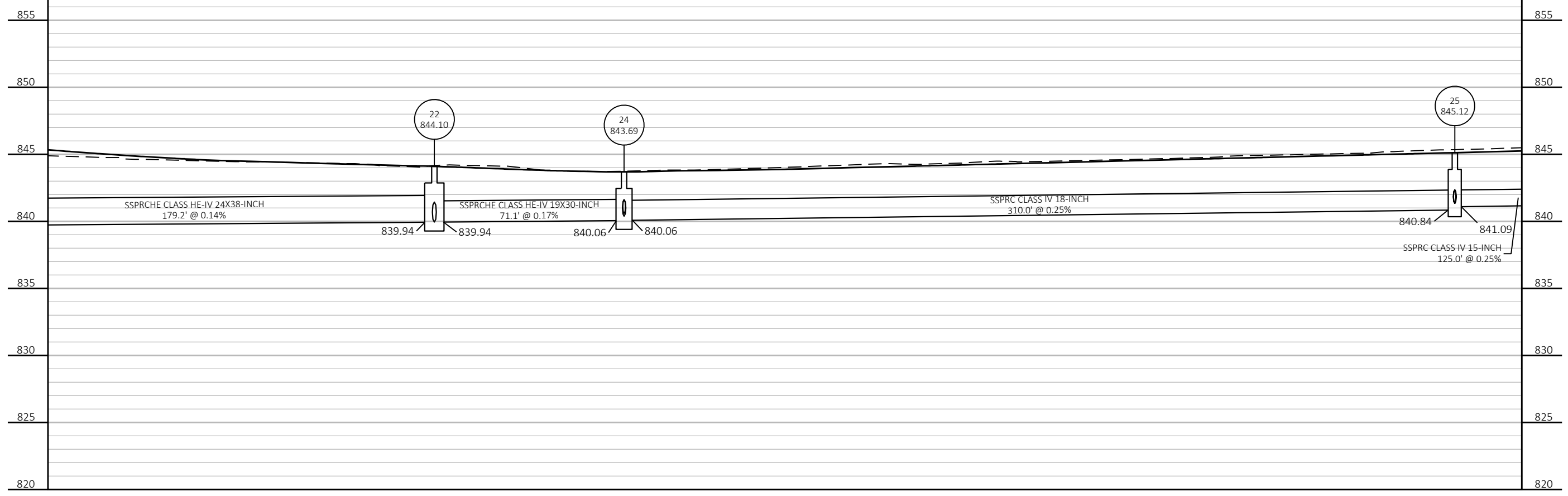
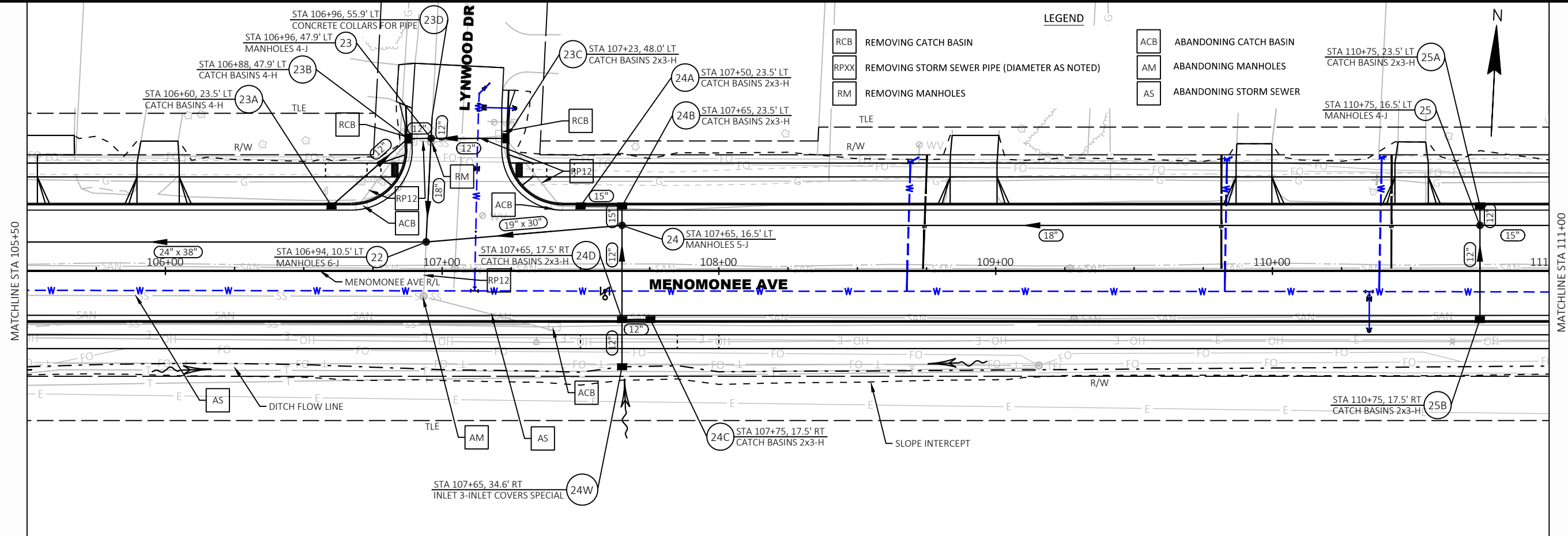
COUNTY: WAUKESHA

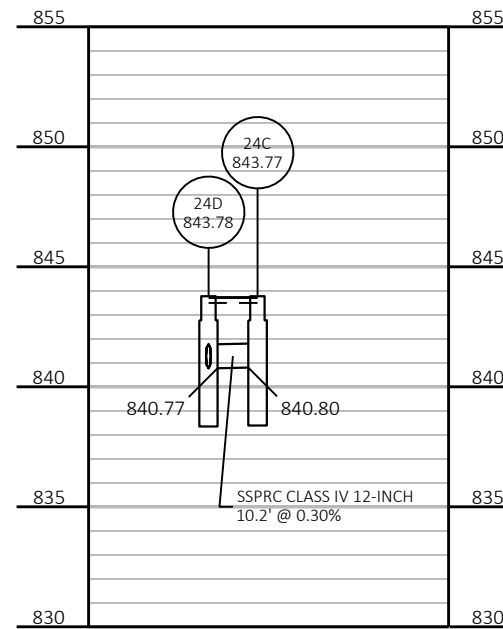
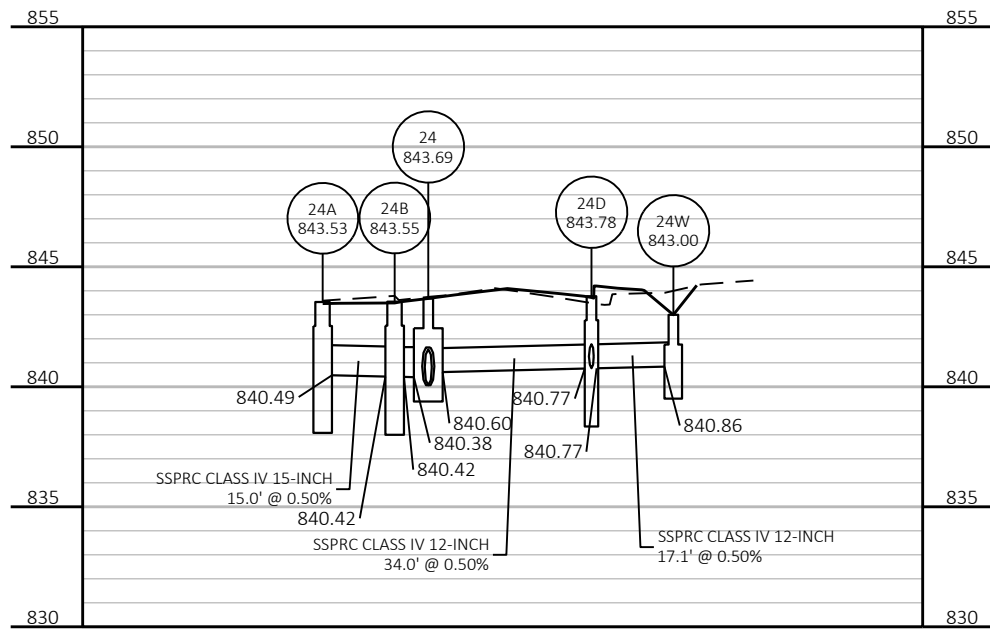
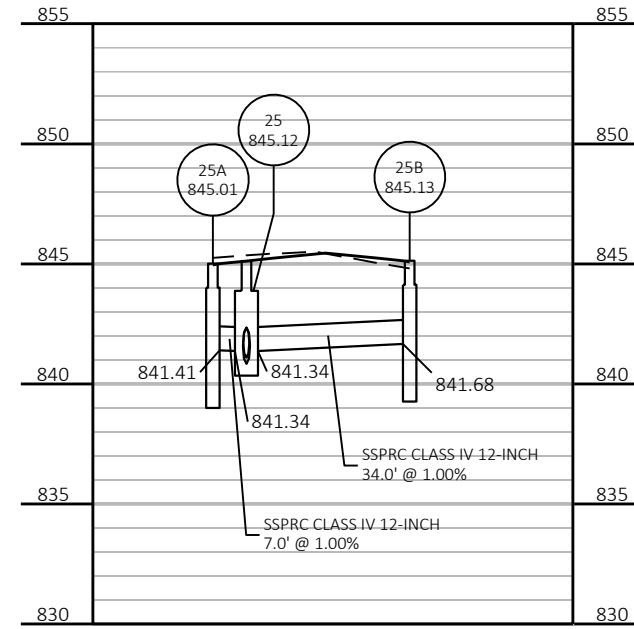
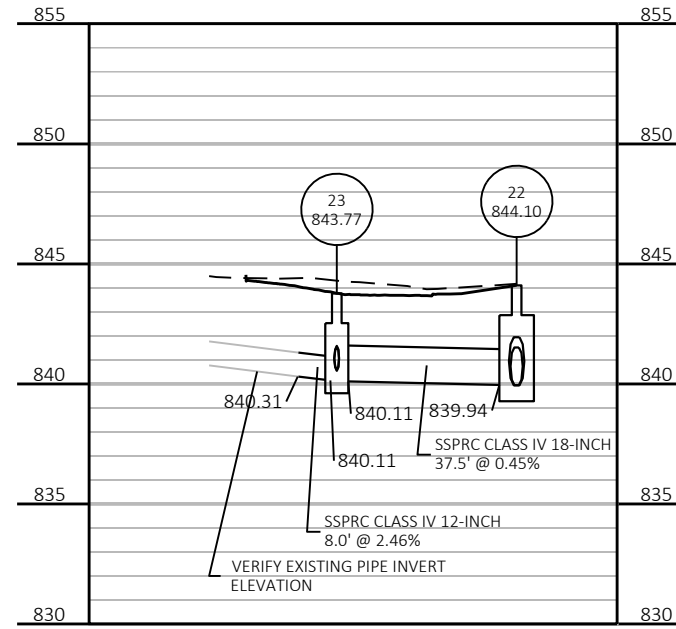
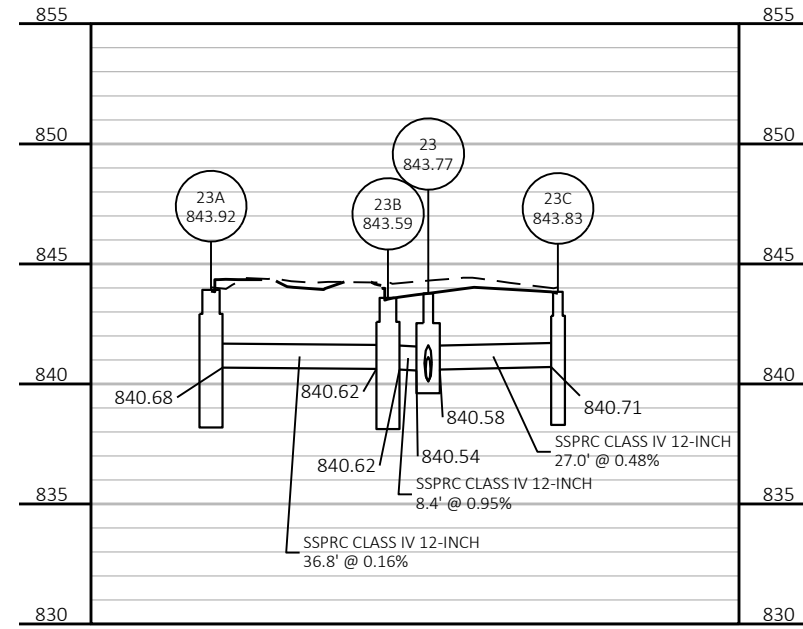
STORM SEWER

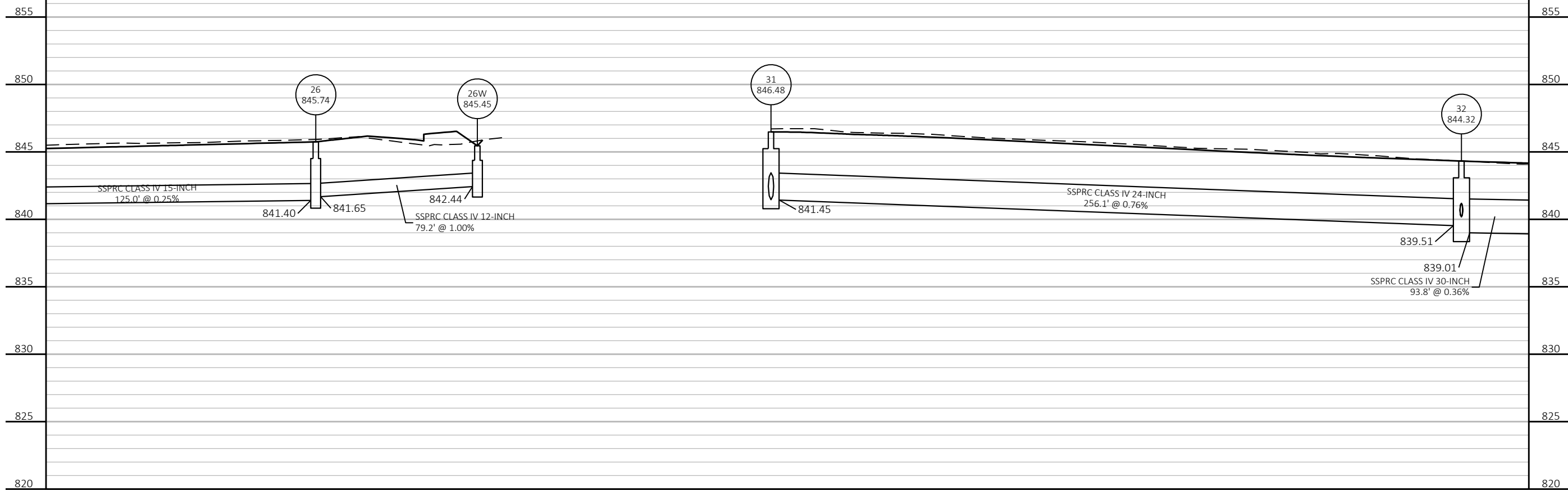
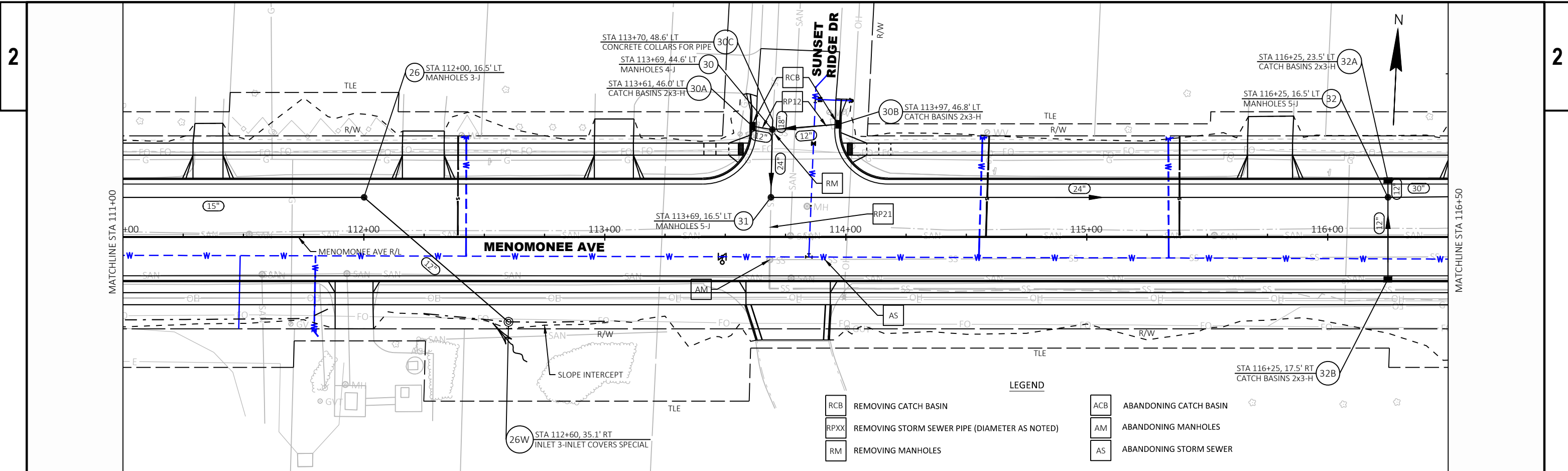
SHEET

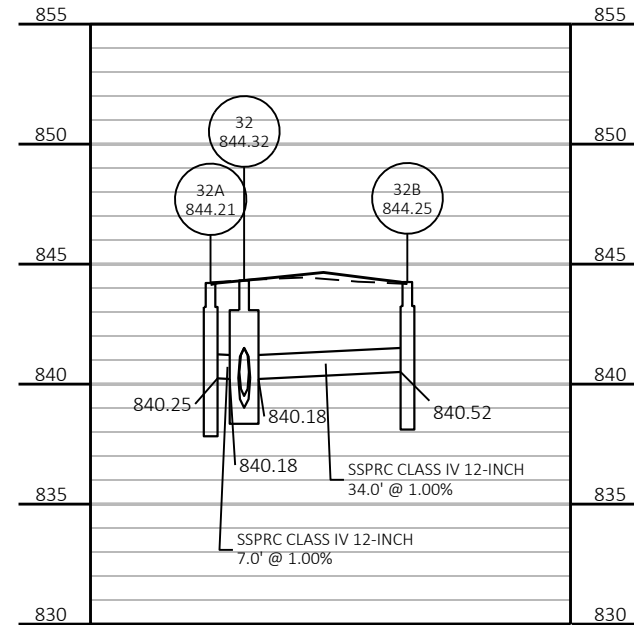
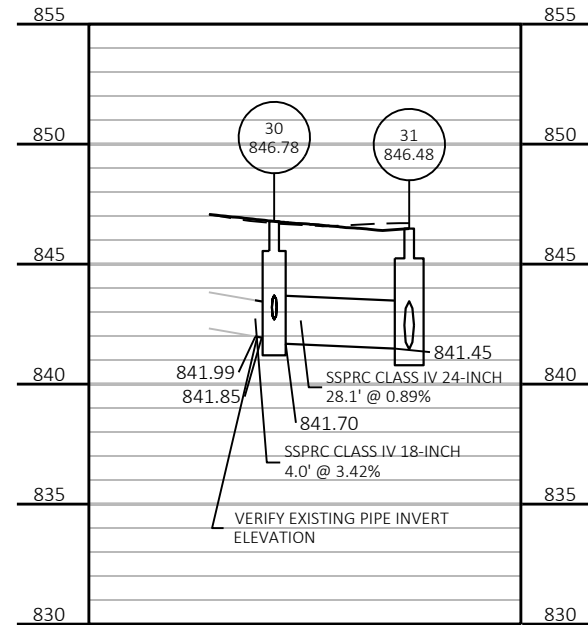
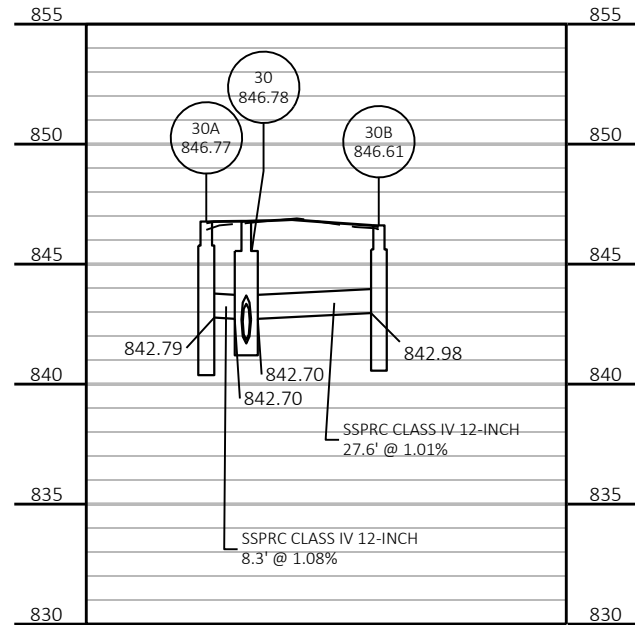
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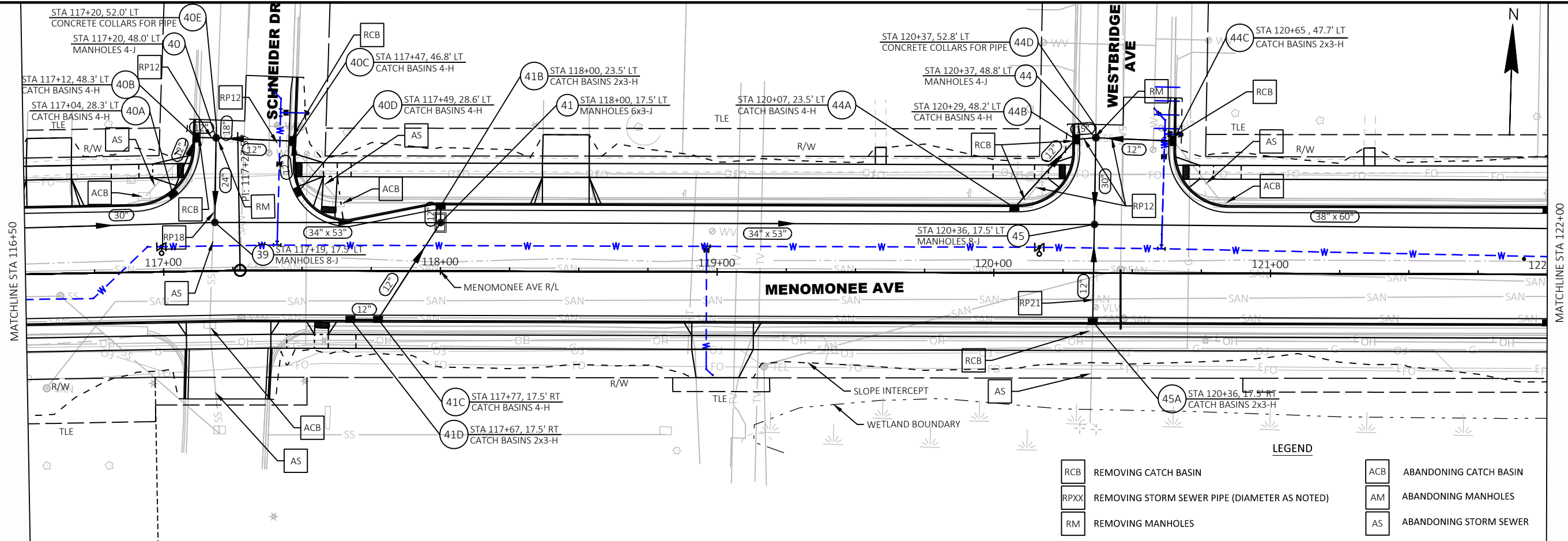






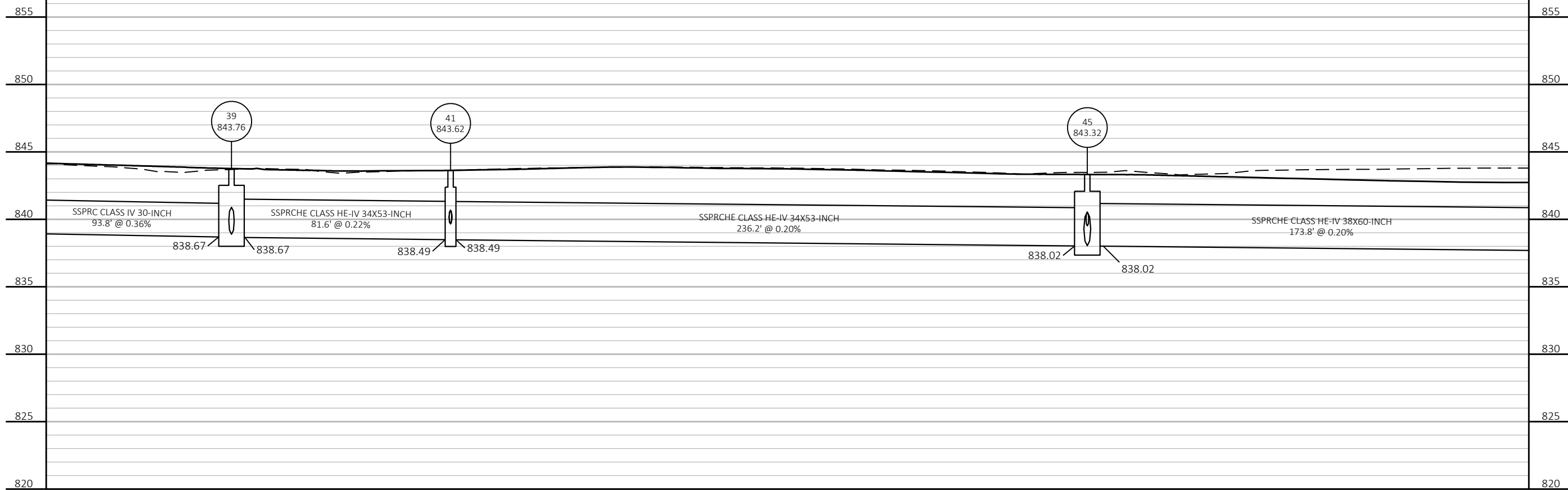


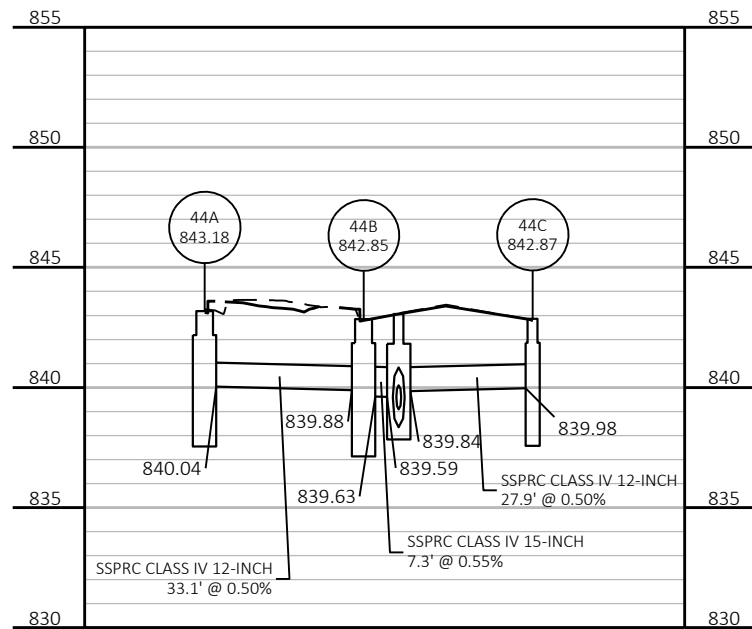
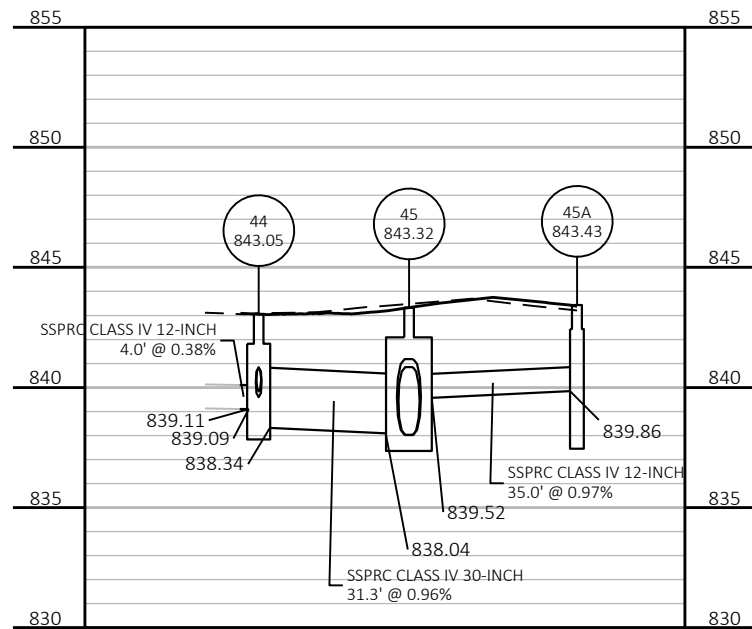
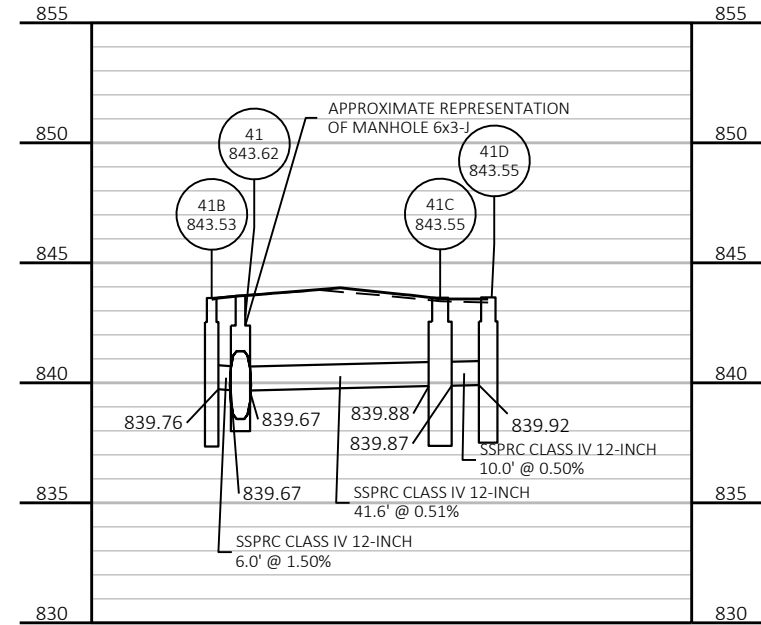
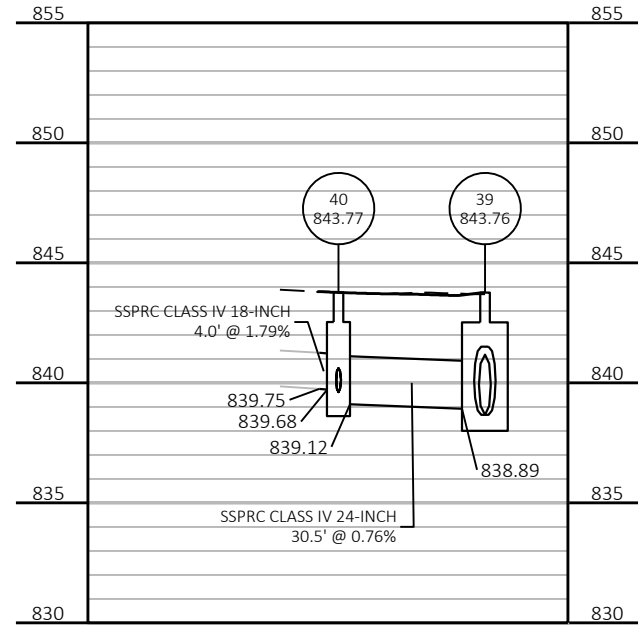
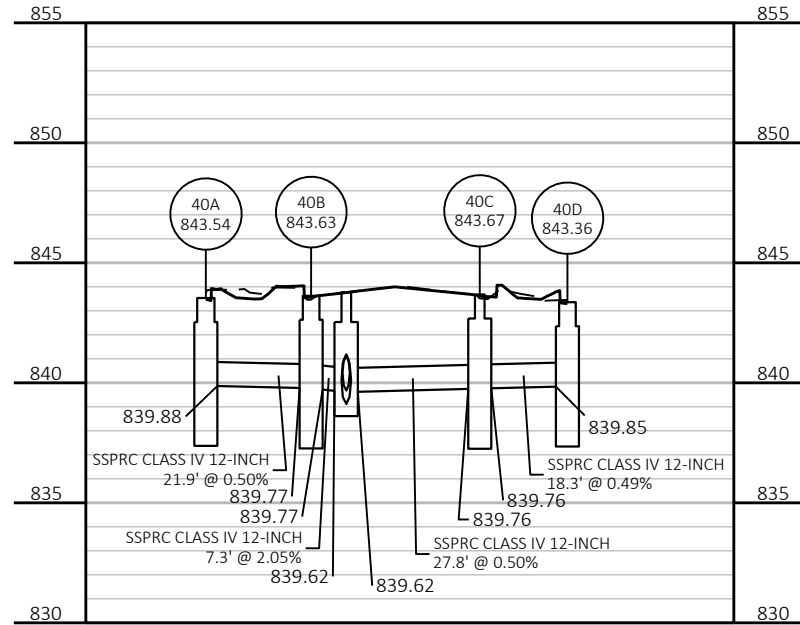


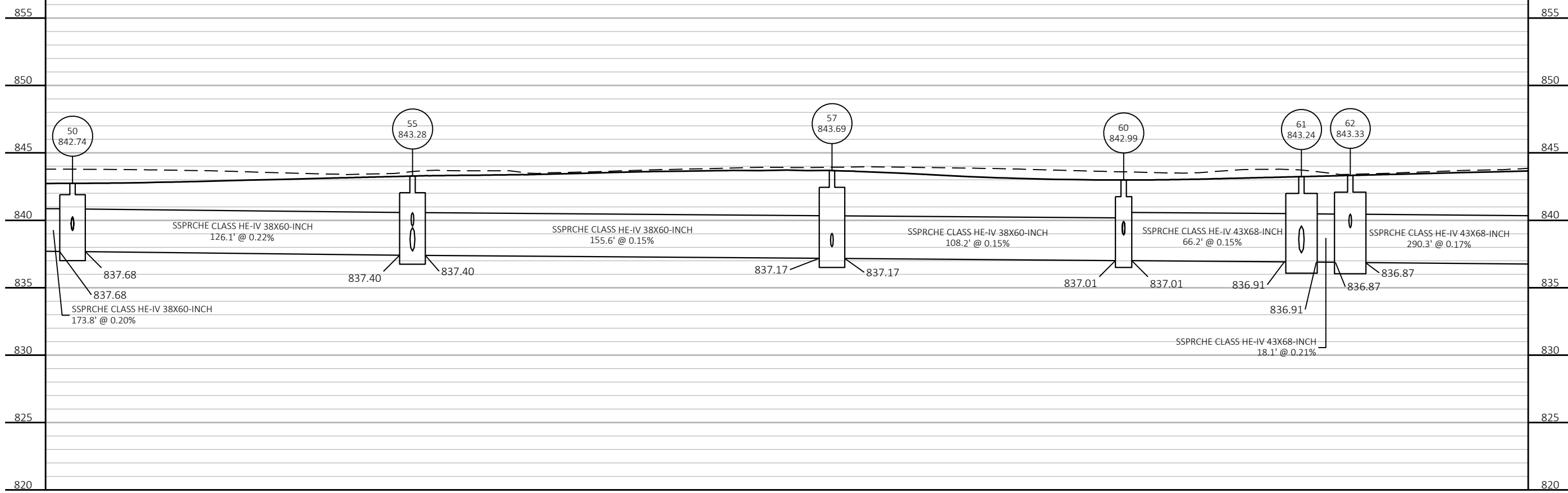
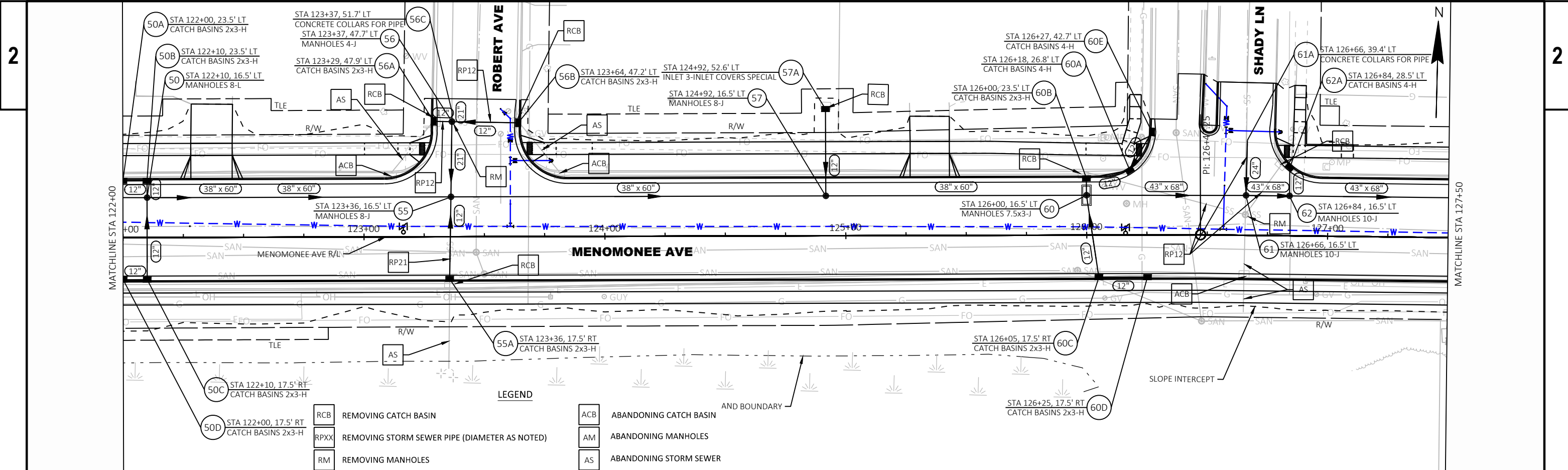


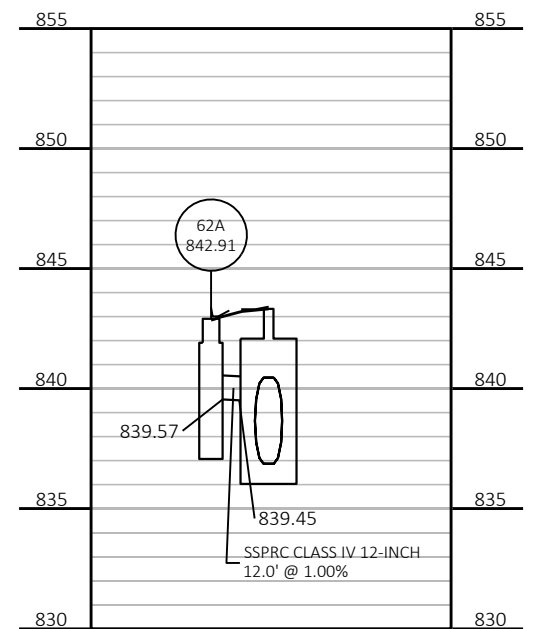
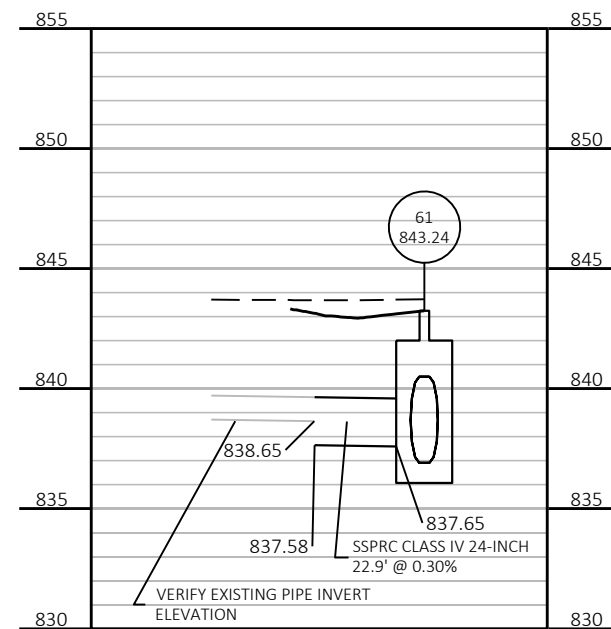
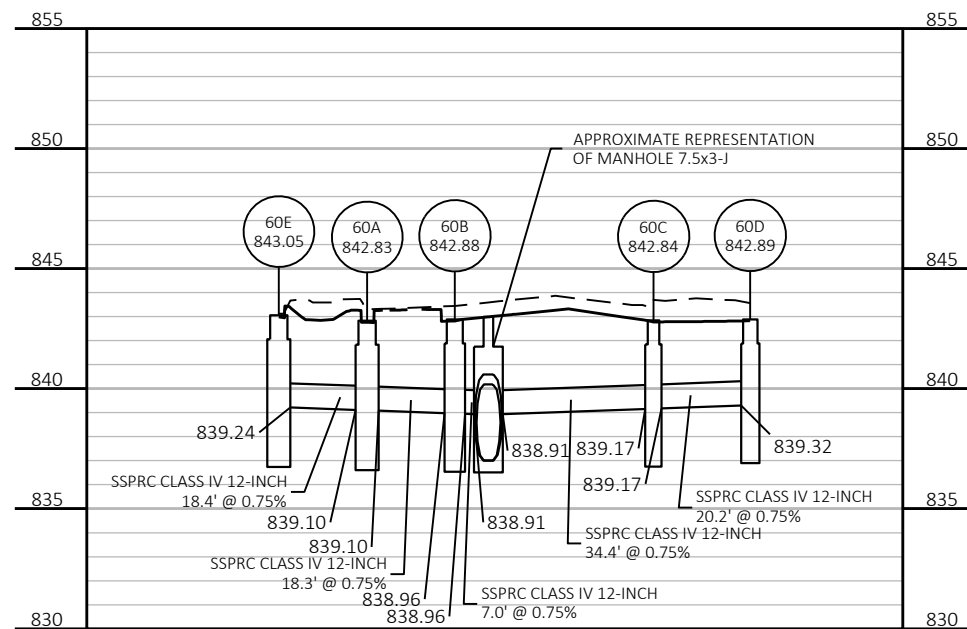
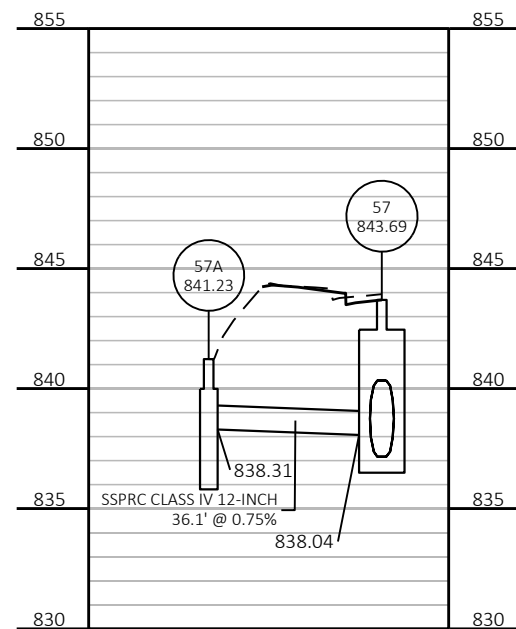
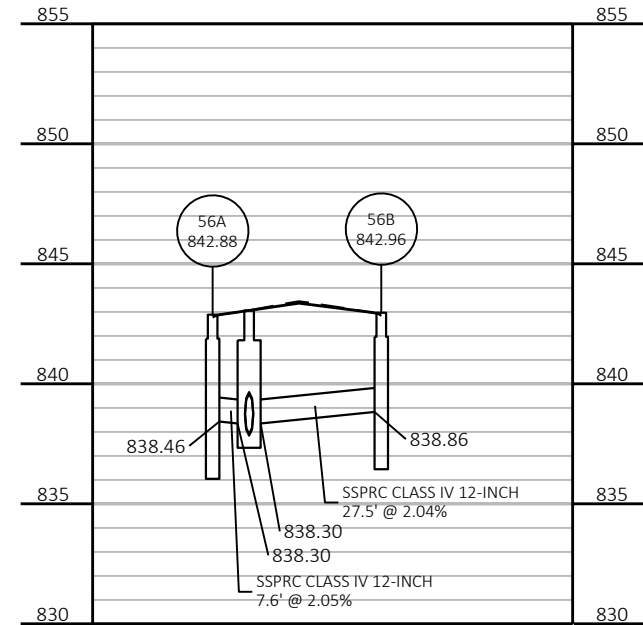
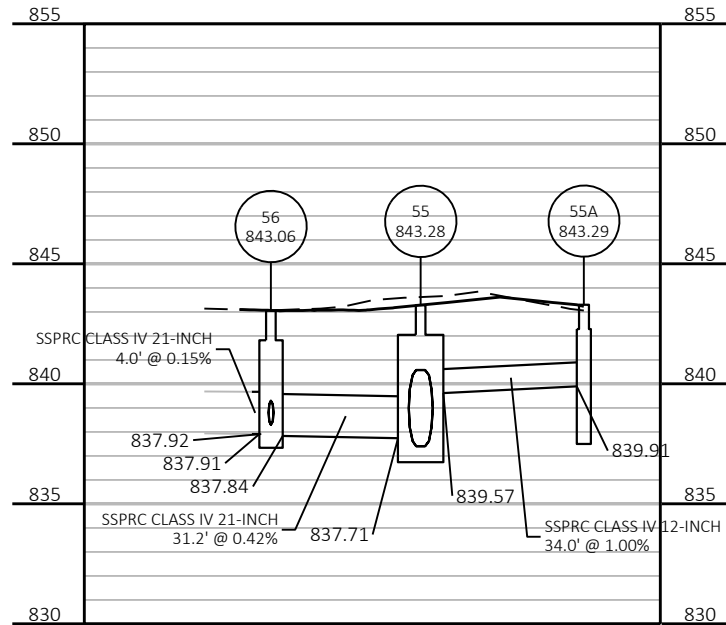
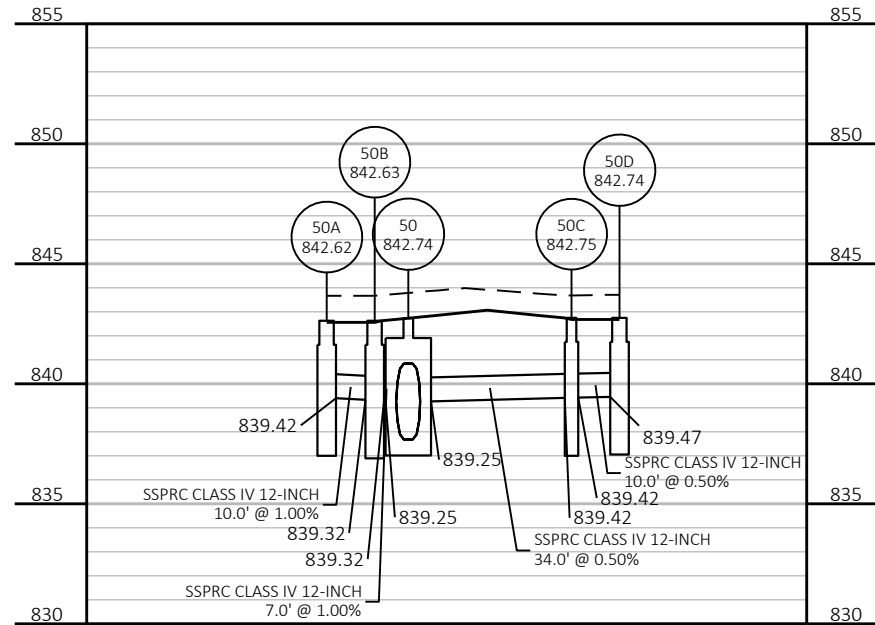
LEGEND

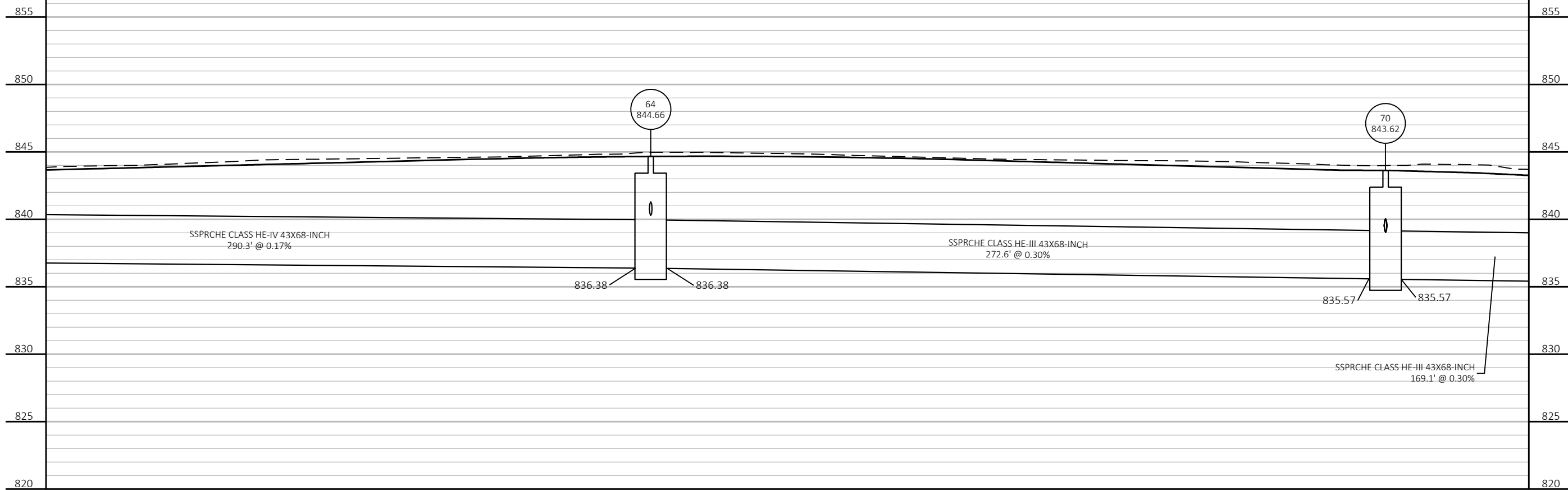
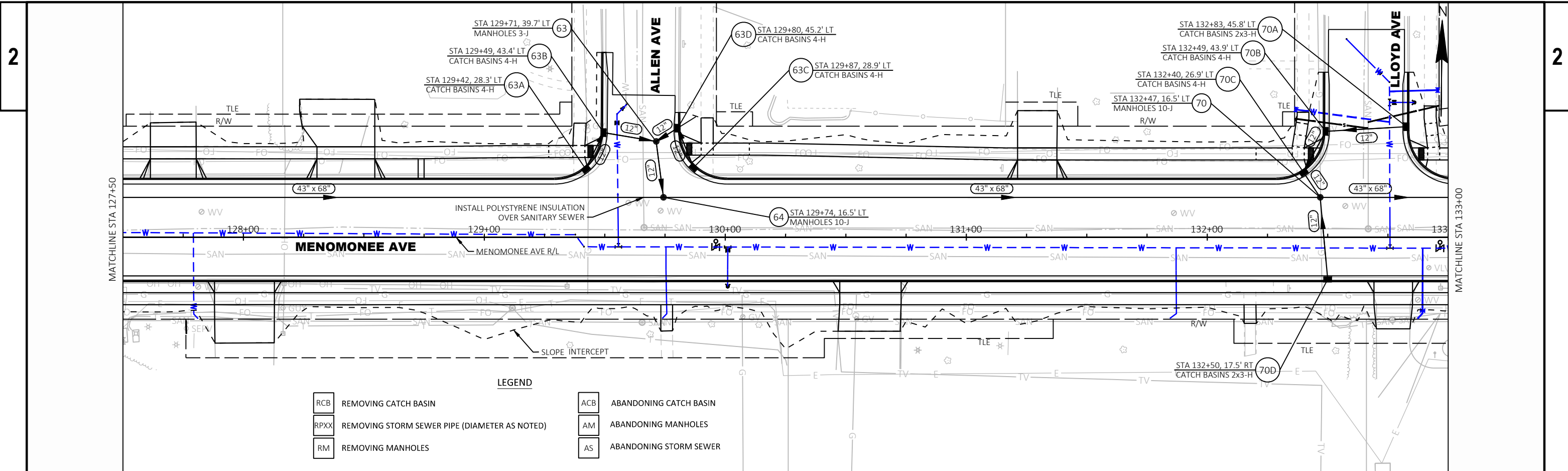
RCB	REMOVING CATCH BASIN	ACB	ABANDONING CATCH BASIN
RPXX	REMOVING STORM SEWER PIPE (DIAMETER AS NOTED)	AM	ABANDONING MANHOLES
RM	REMOVING MANHOLES	AS	ABANDONING STORM SEWER

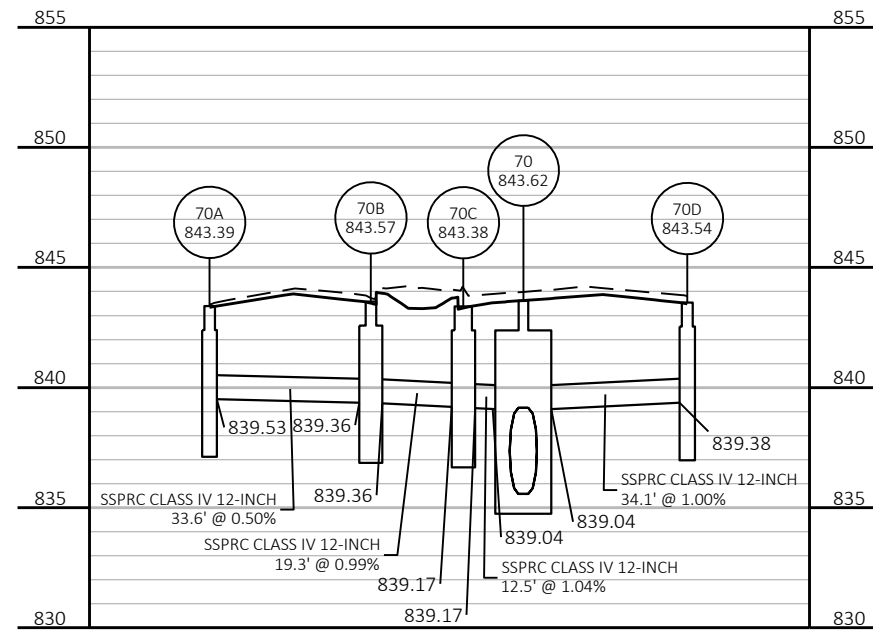
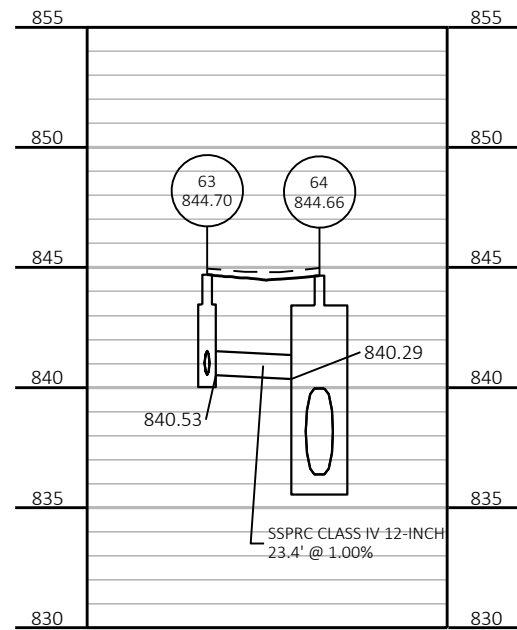
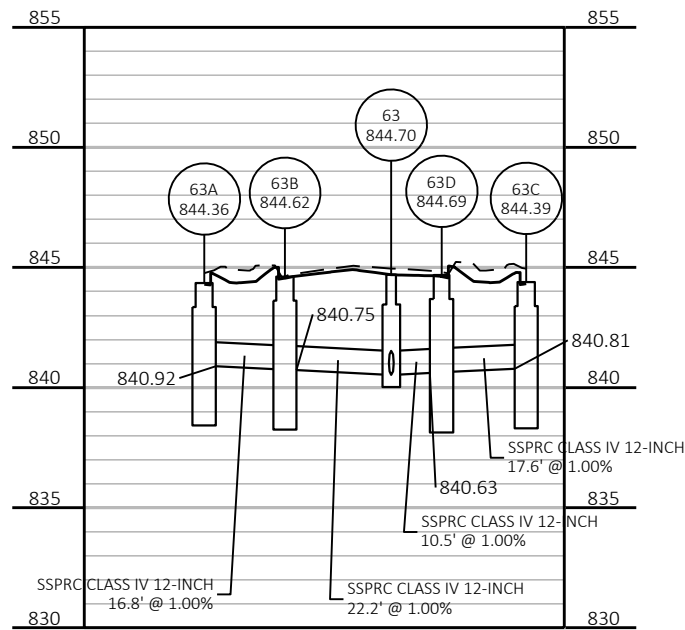


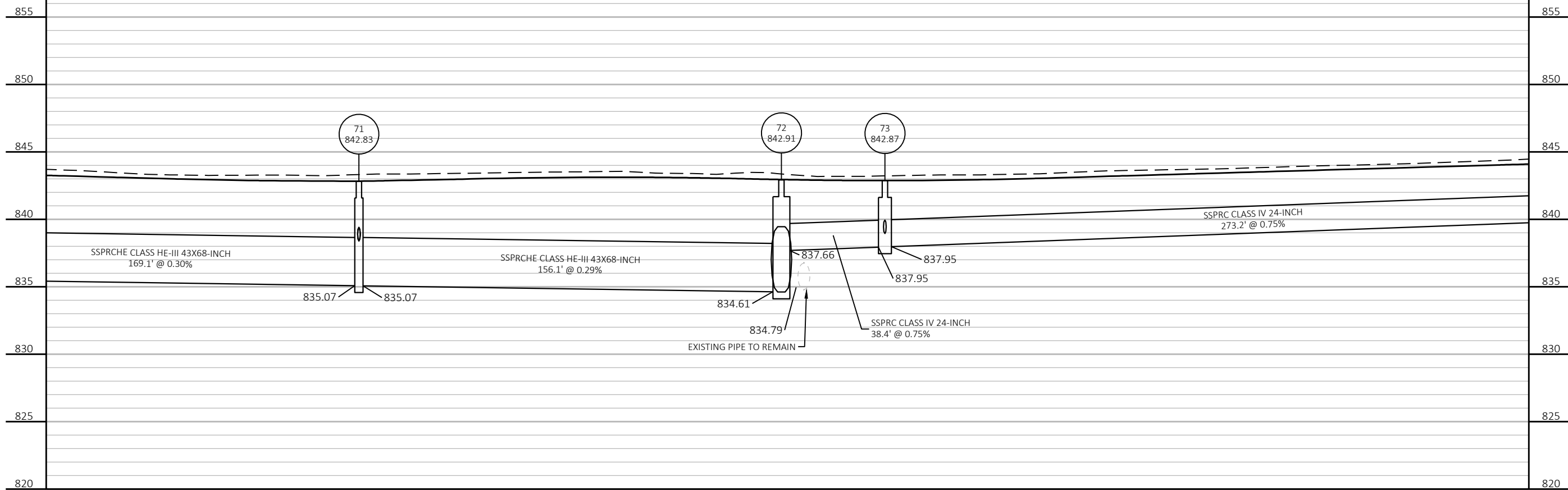
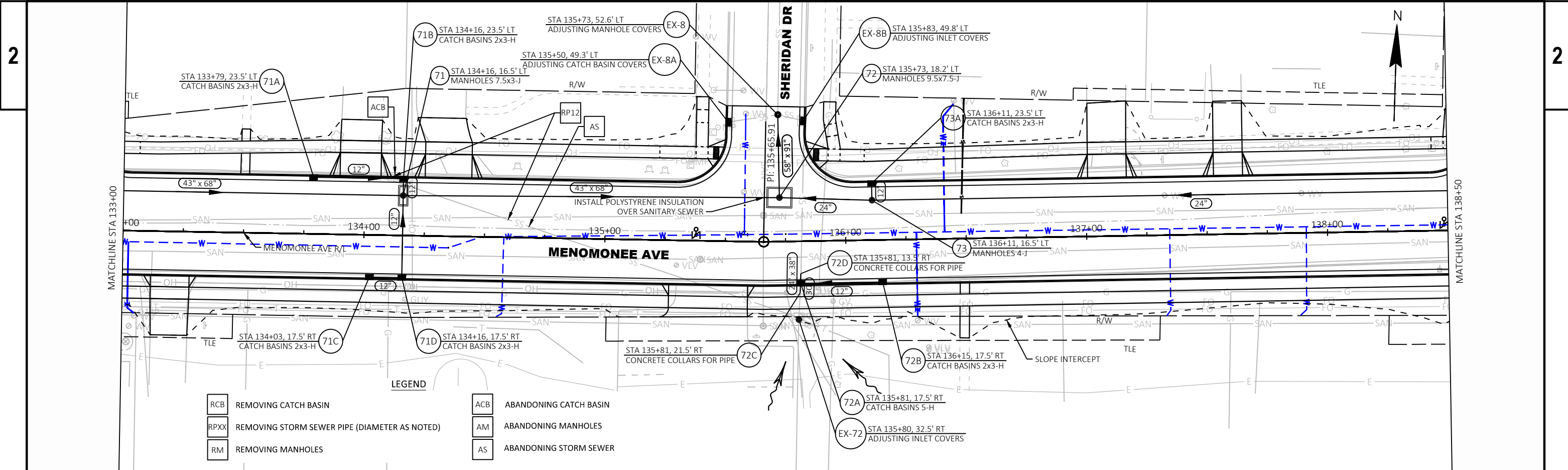


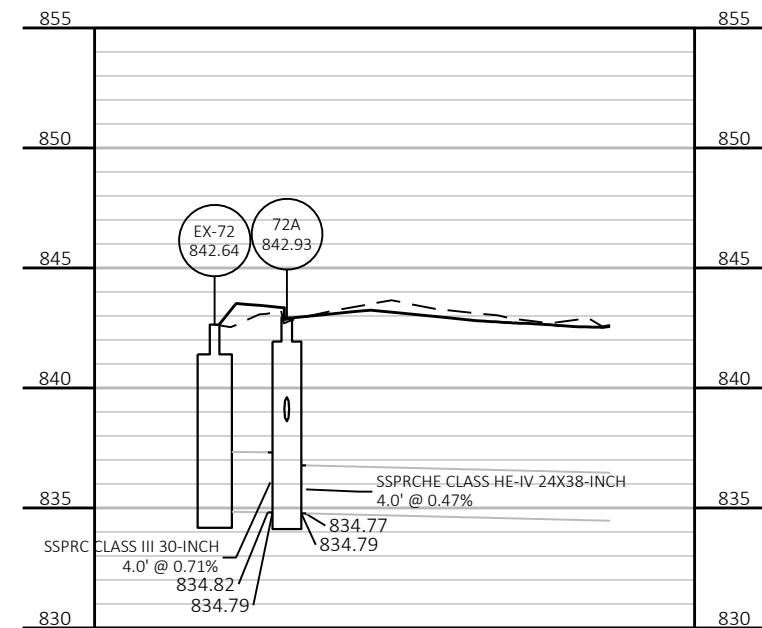
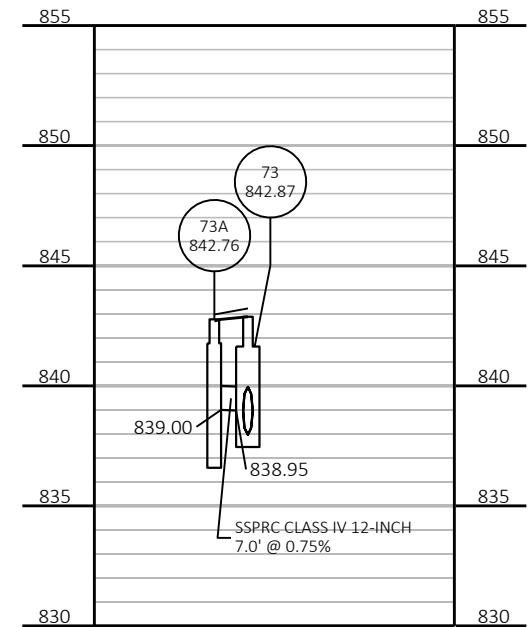
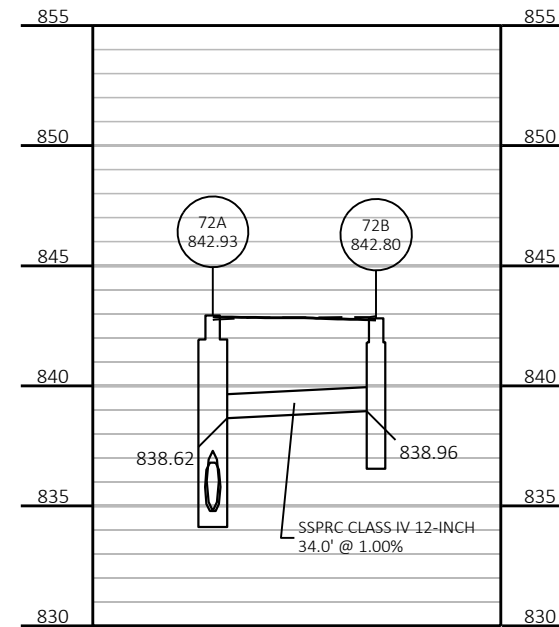
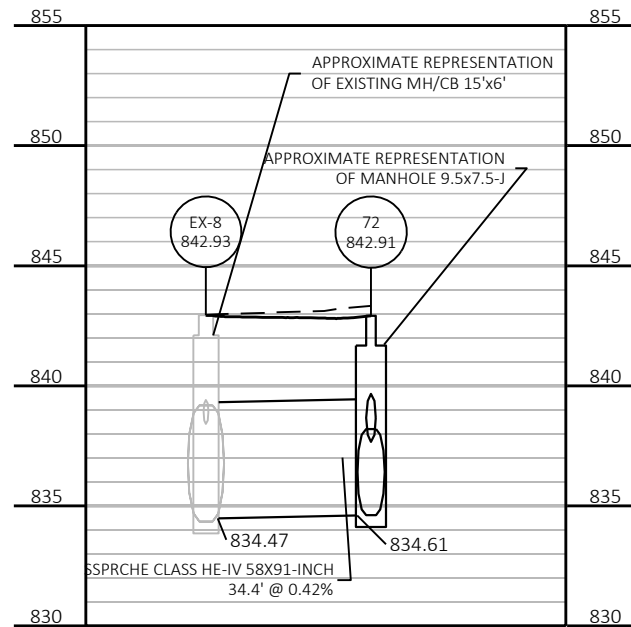
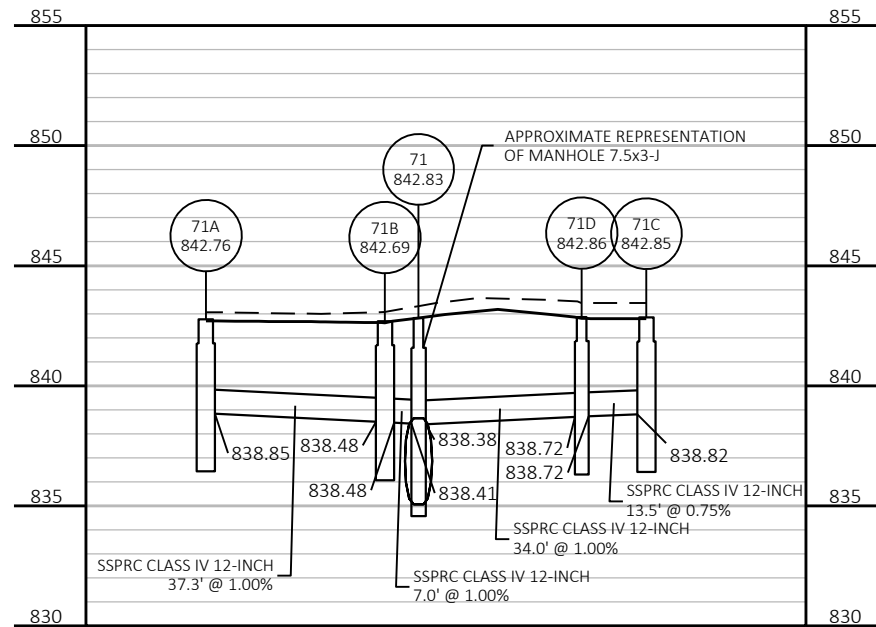


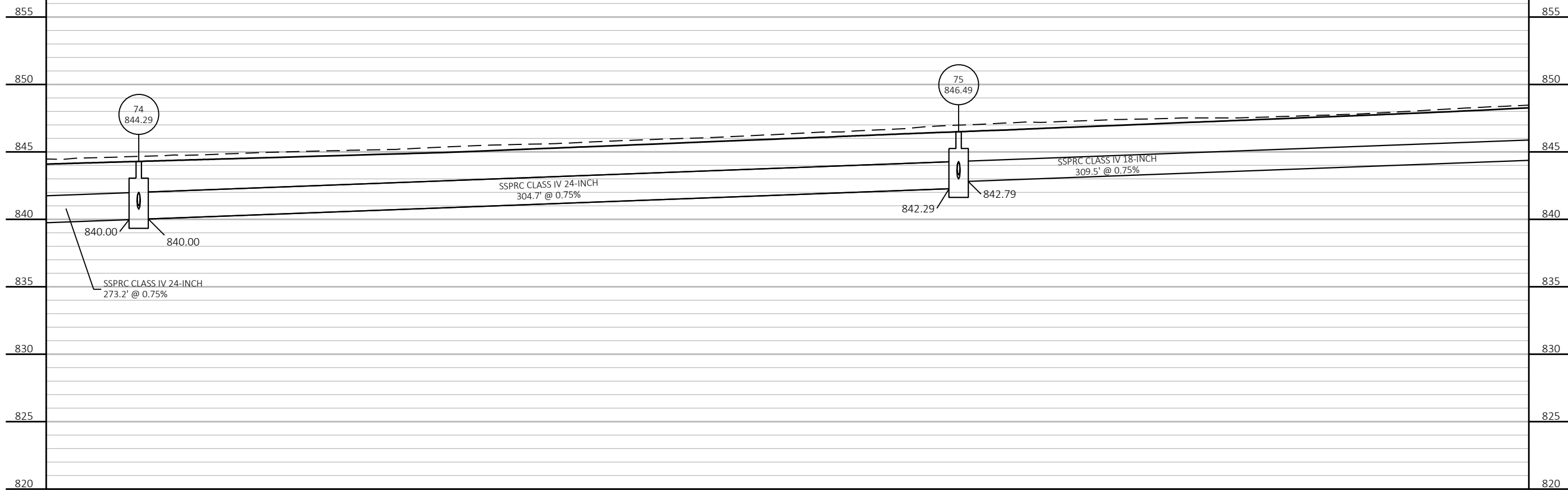
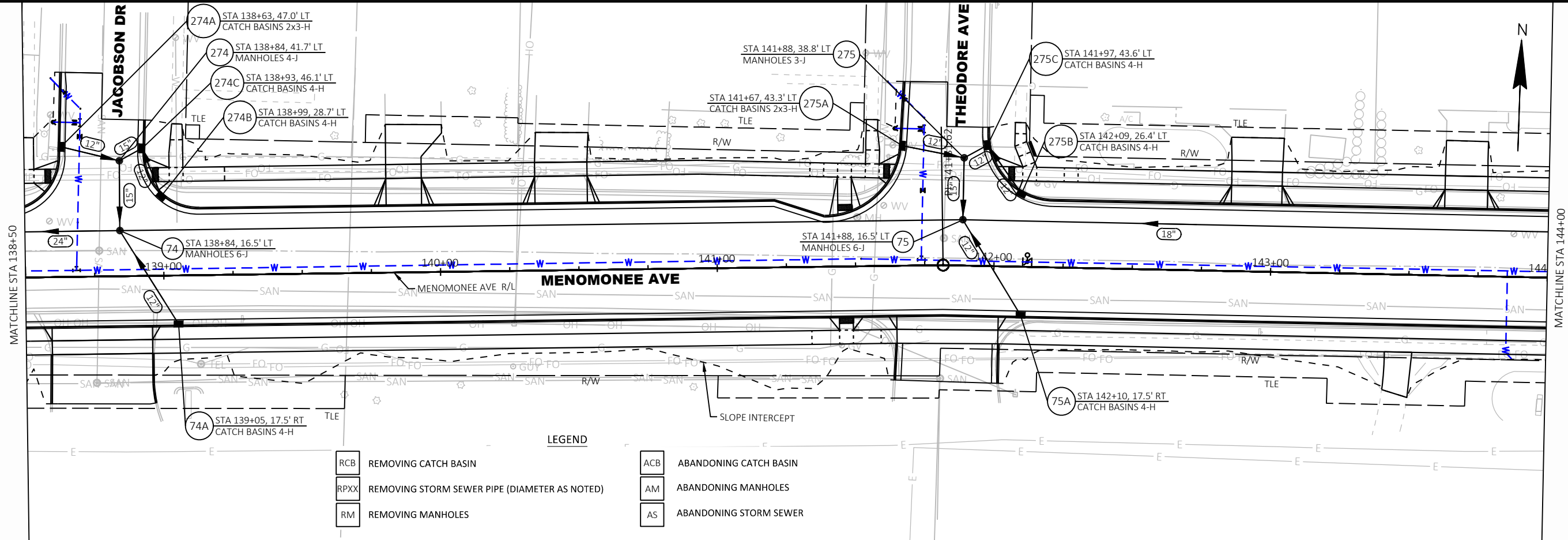


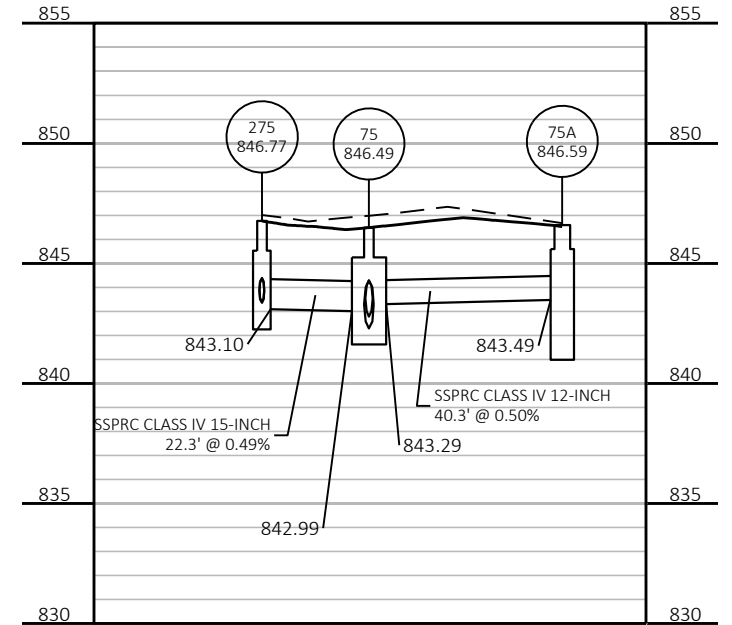
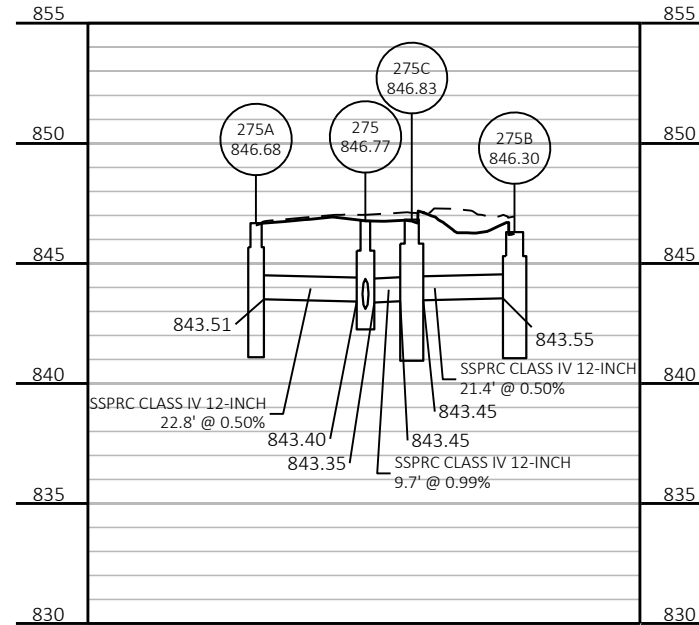
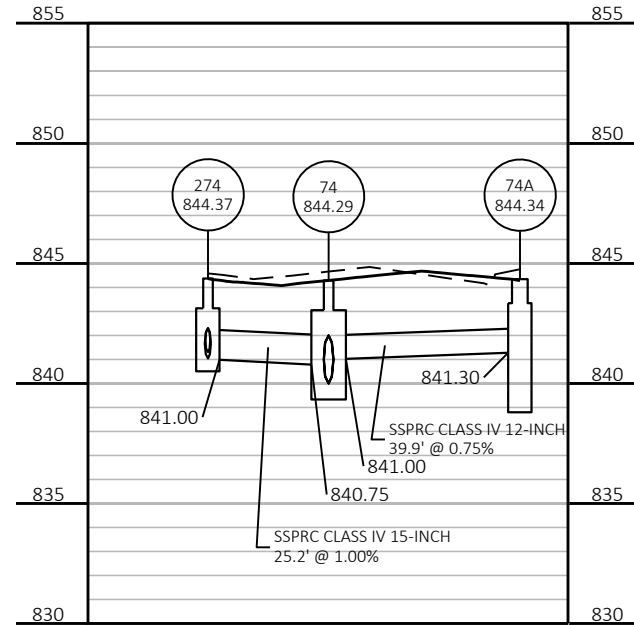
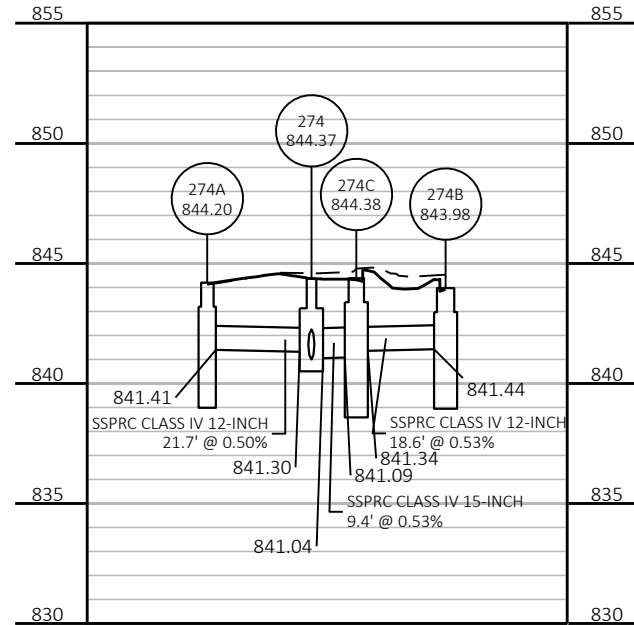


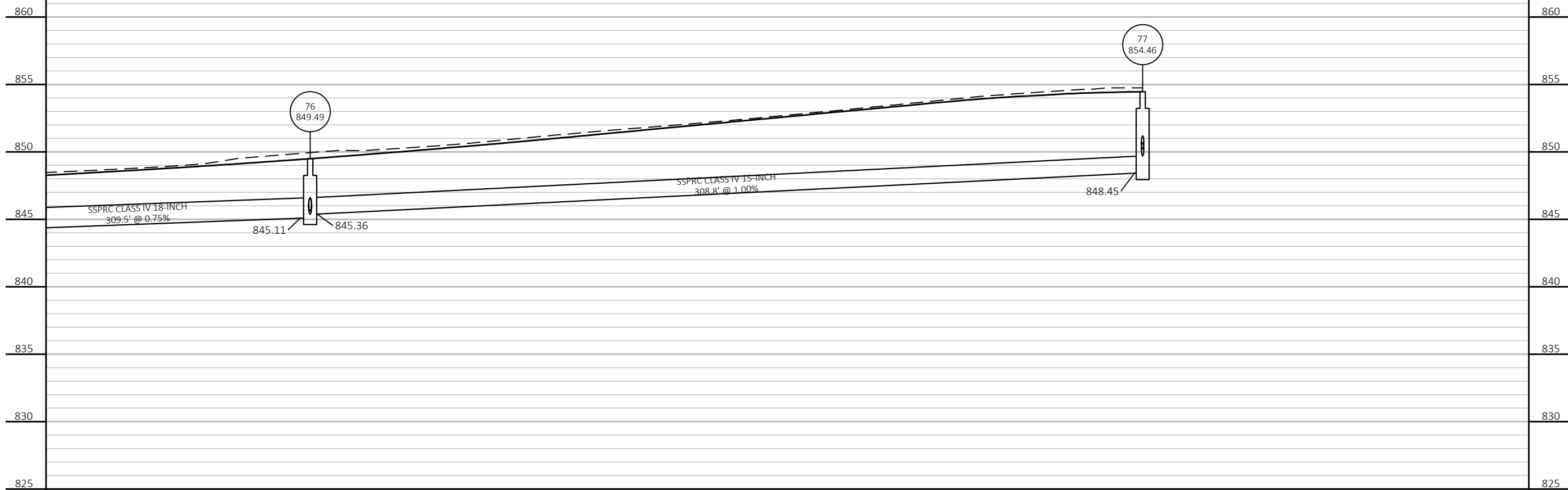
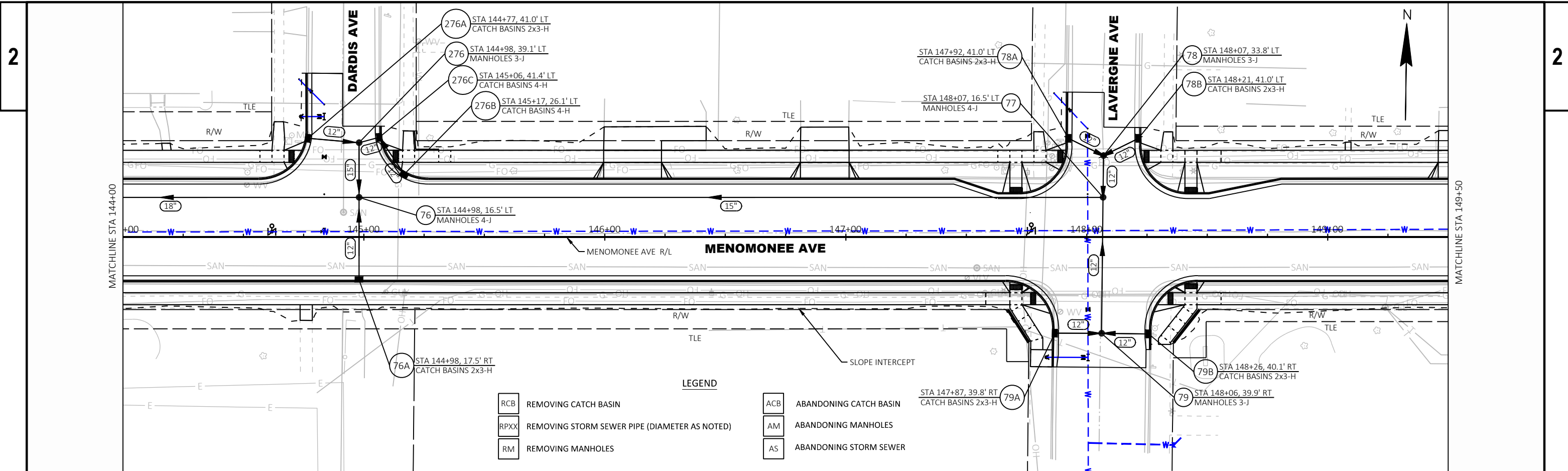


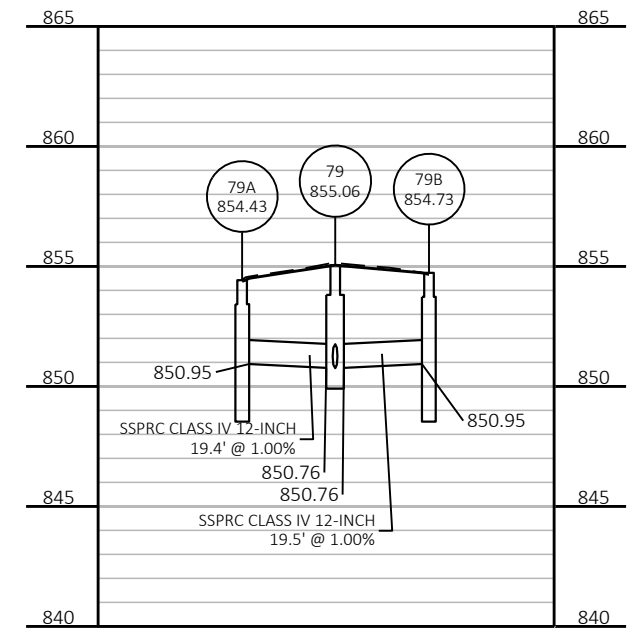
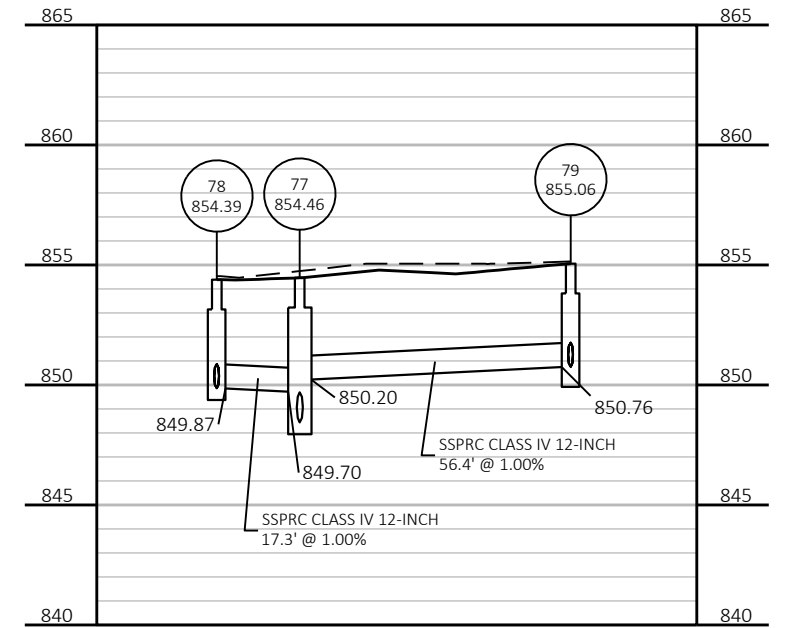
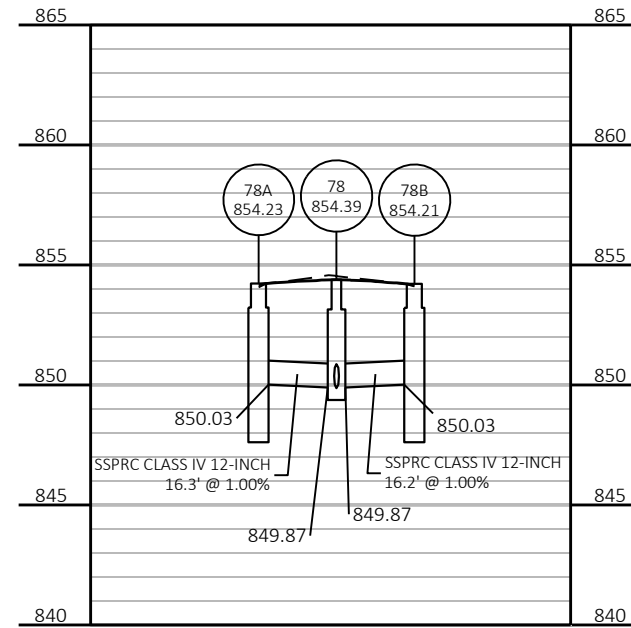
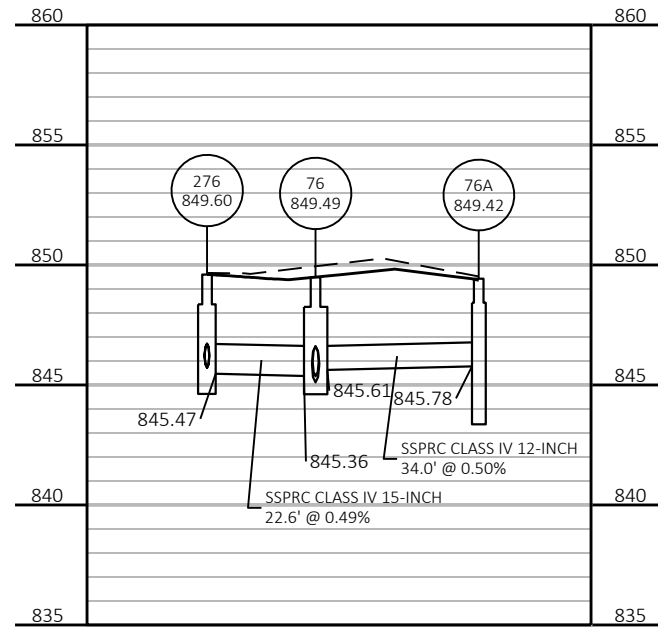
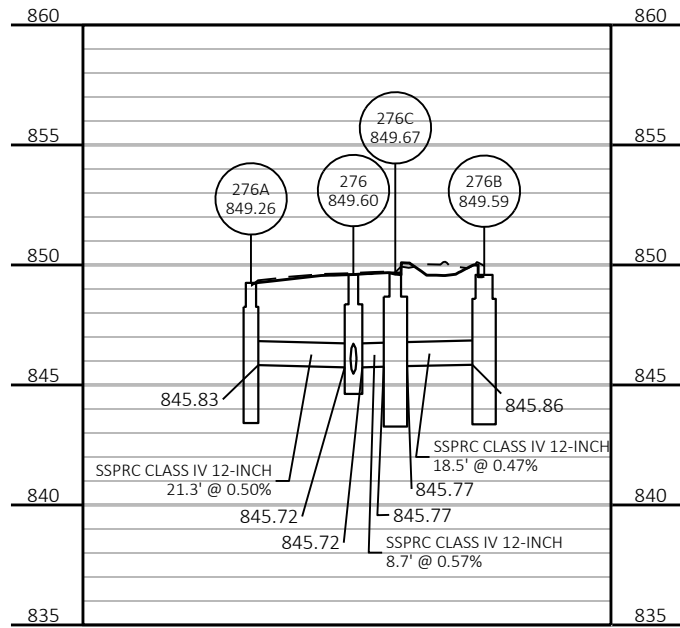


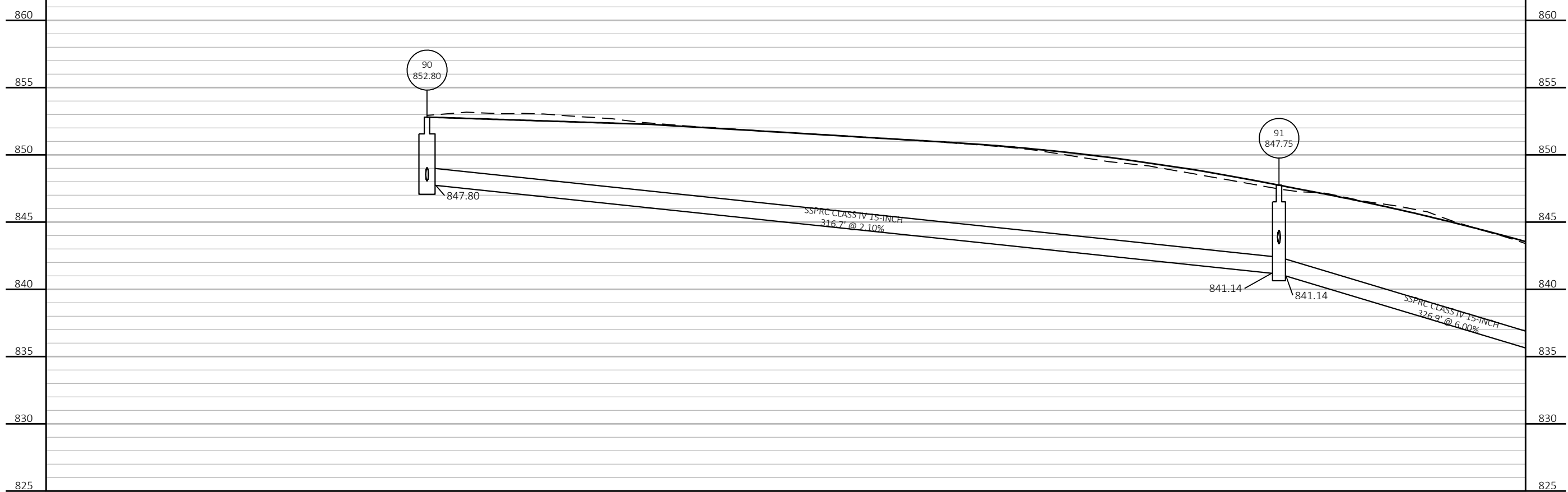
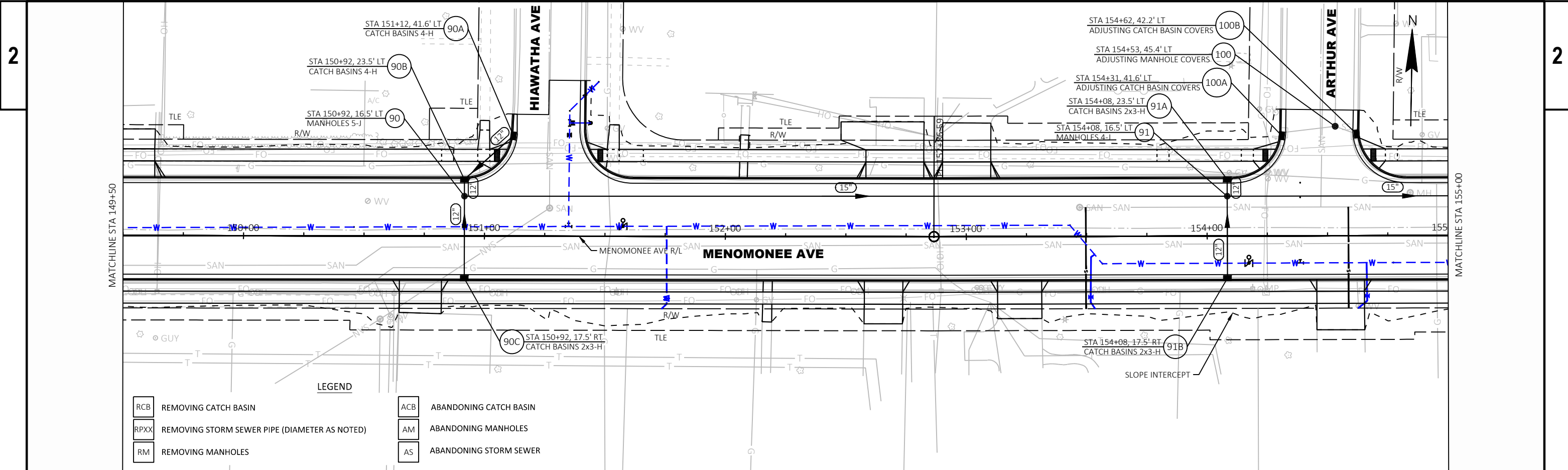


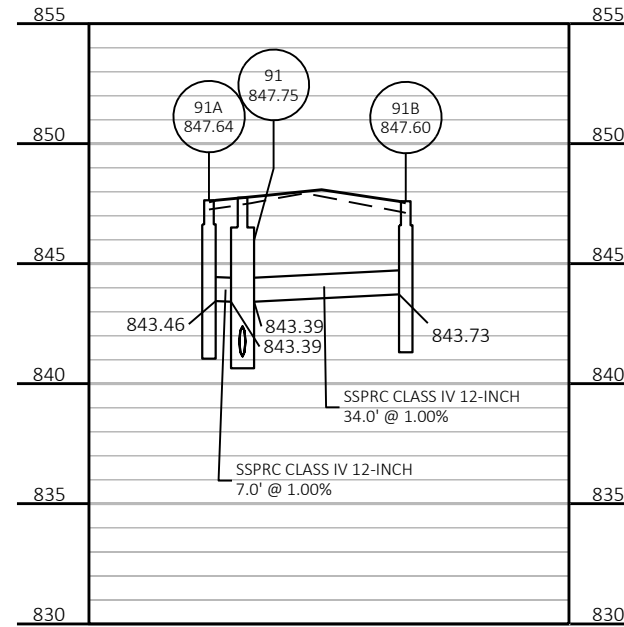
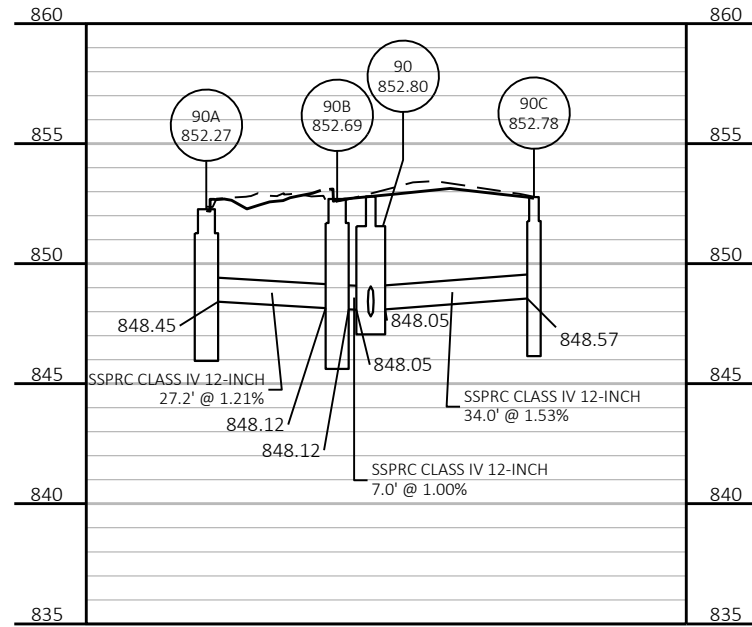


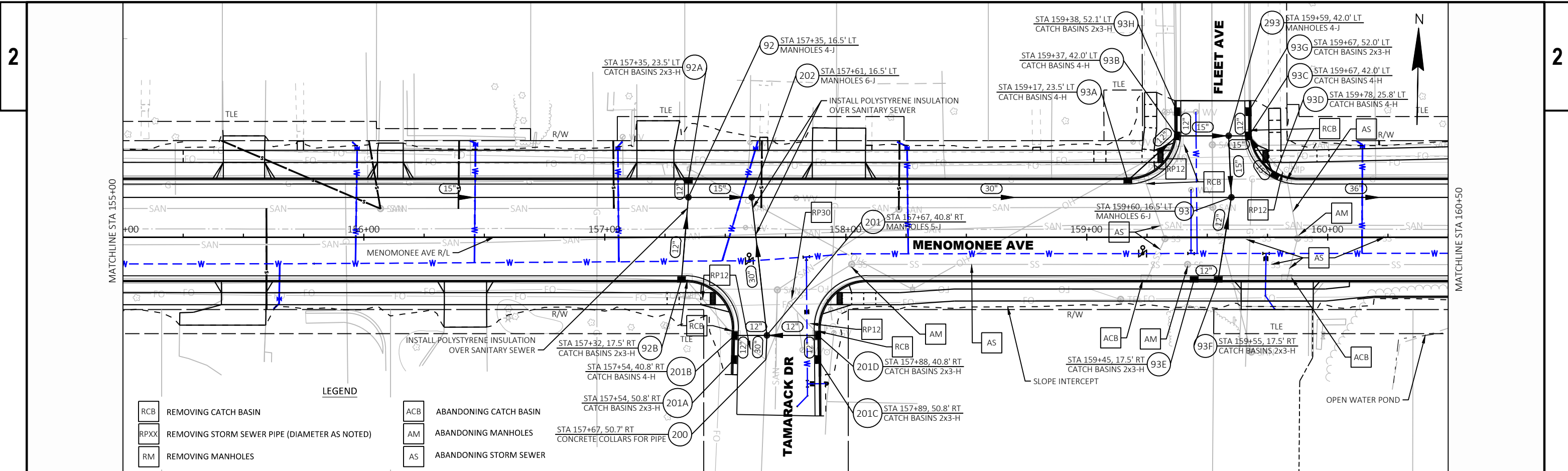




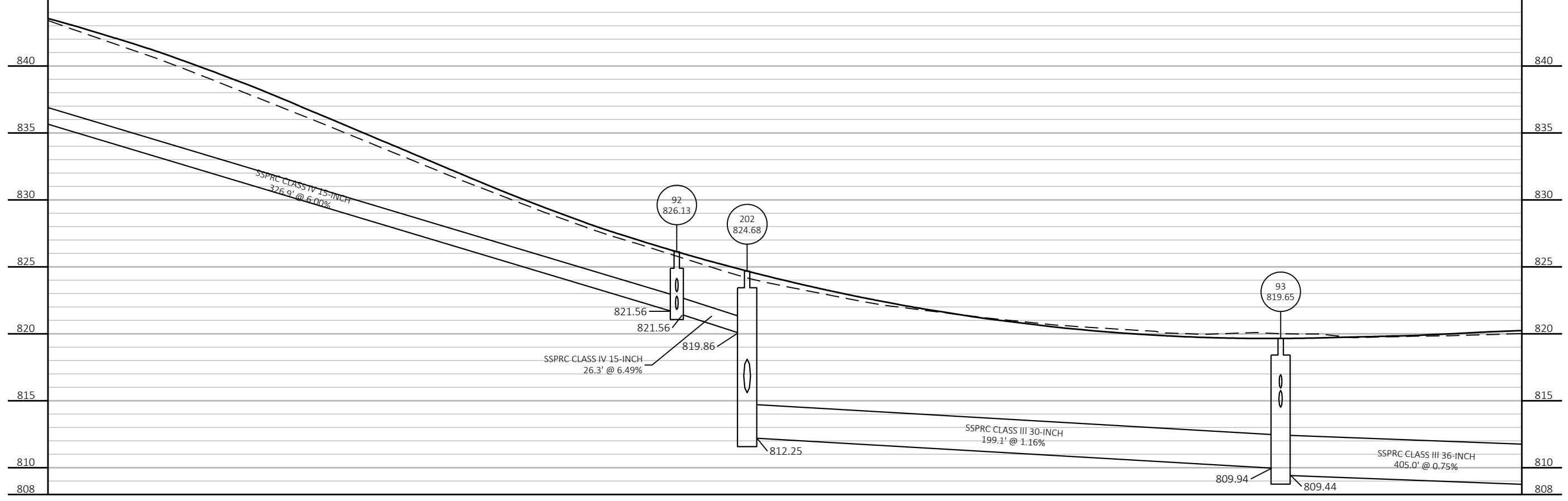


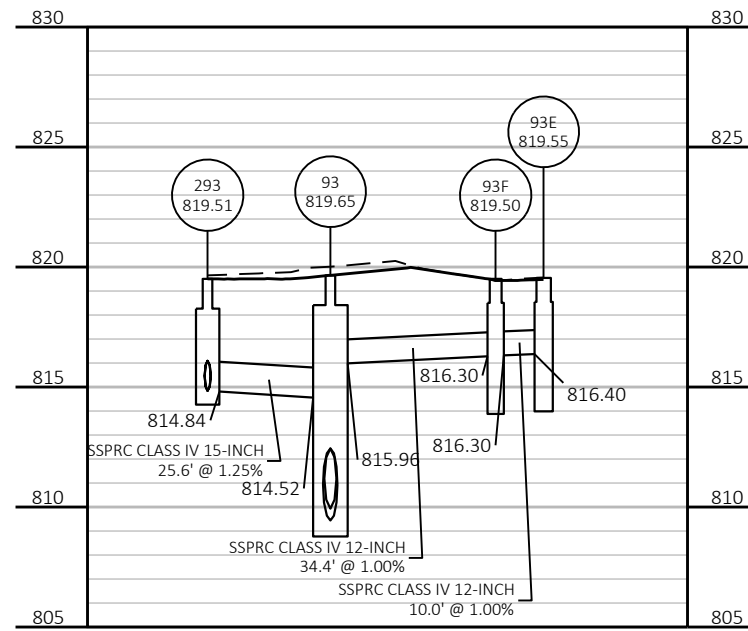
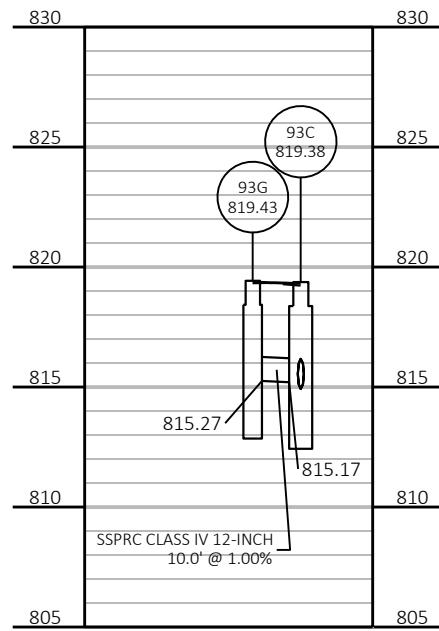
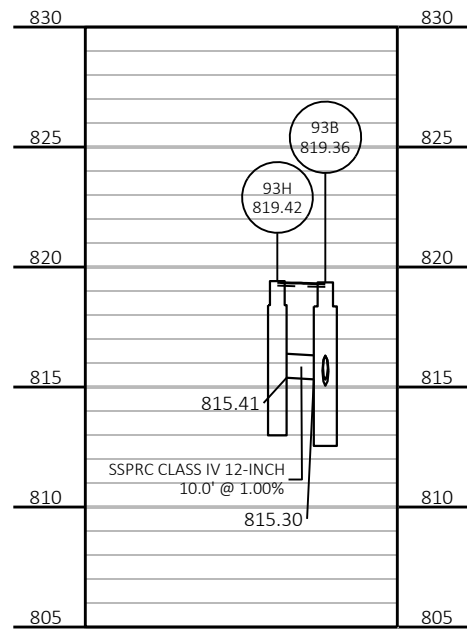
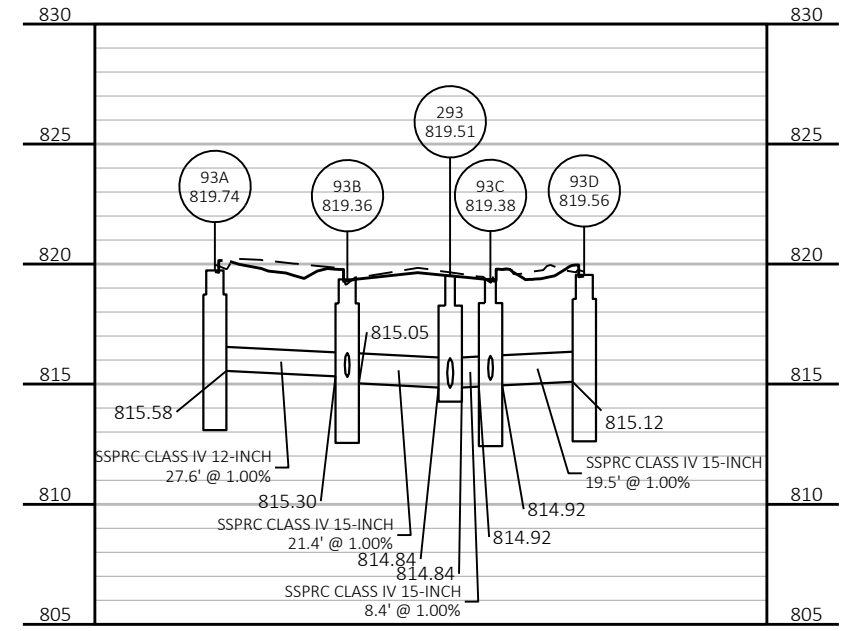
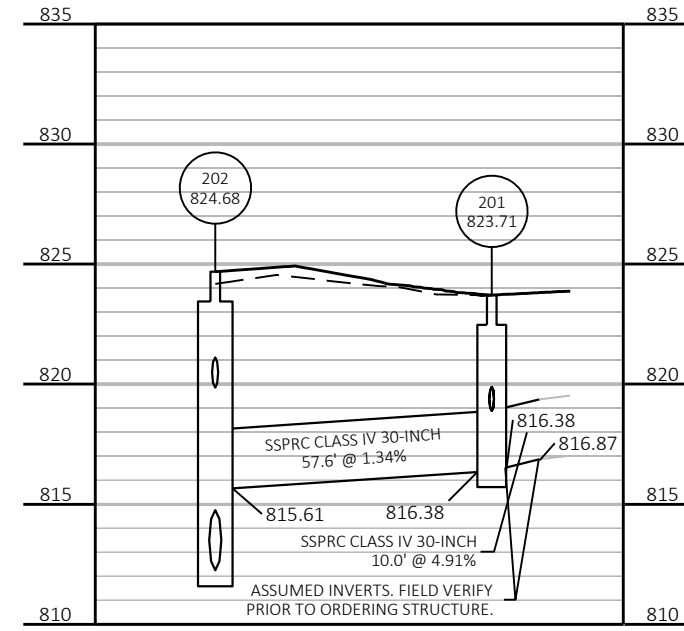
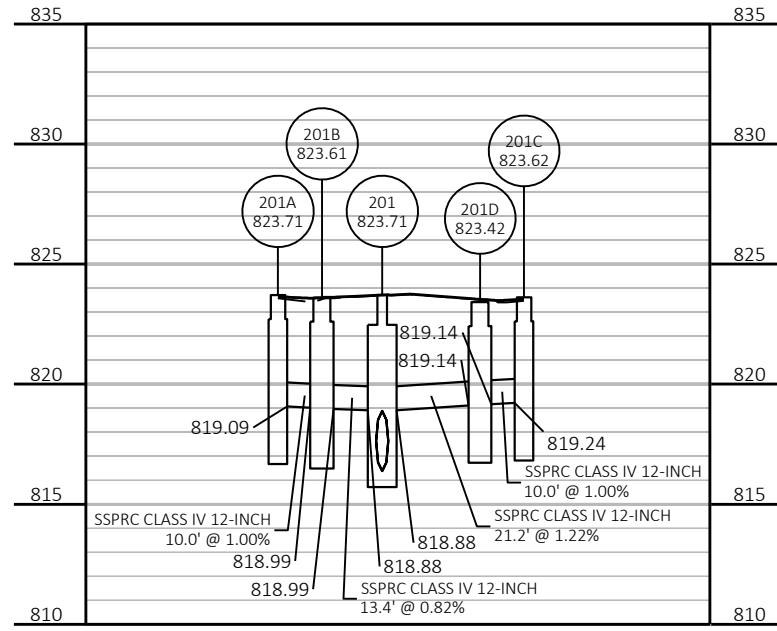
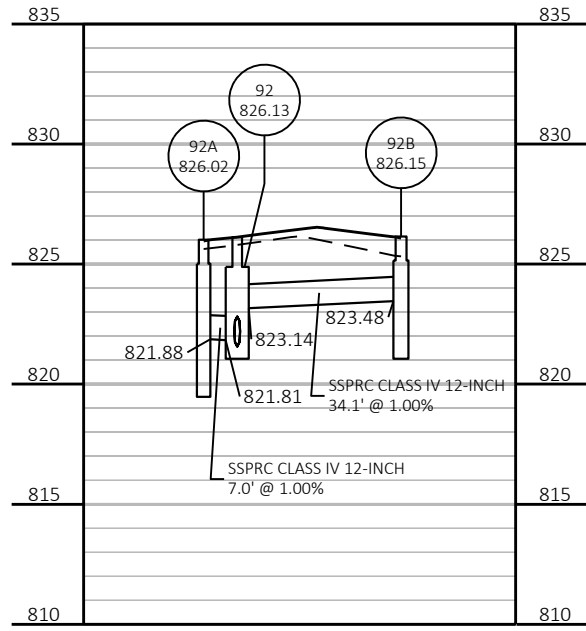


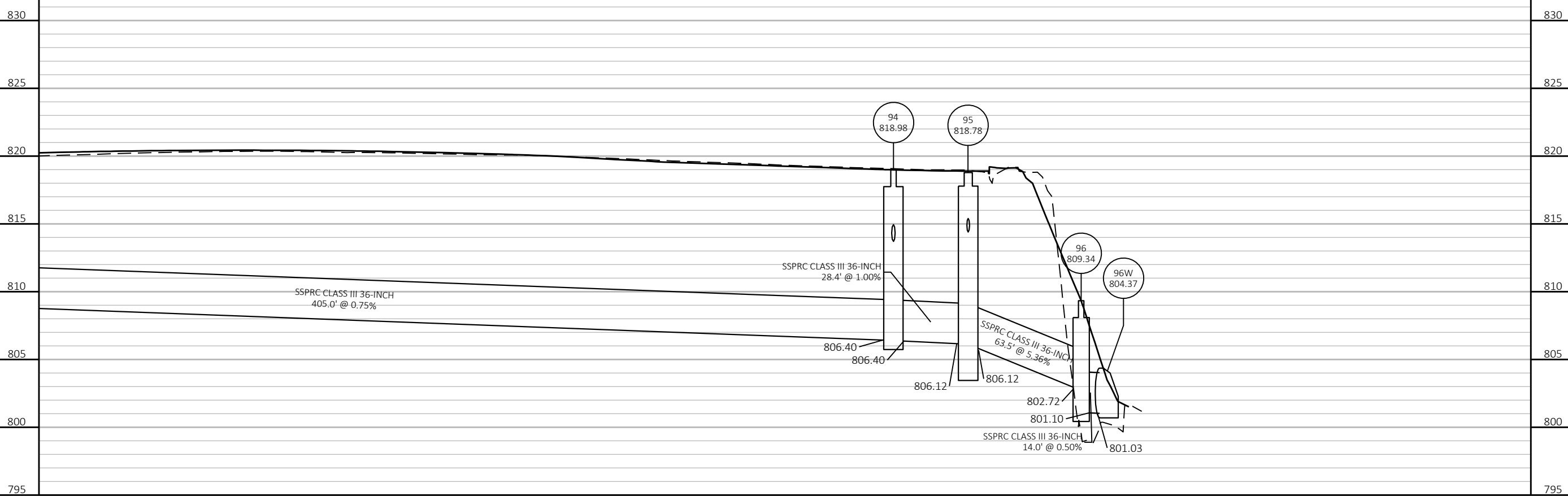
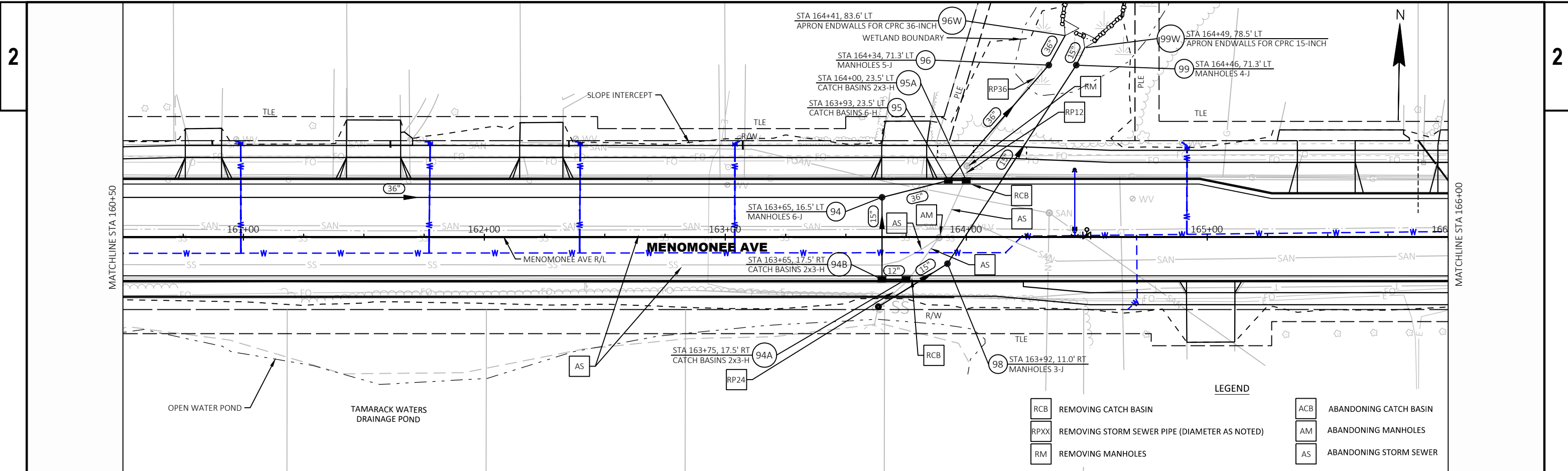




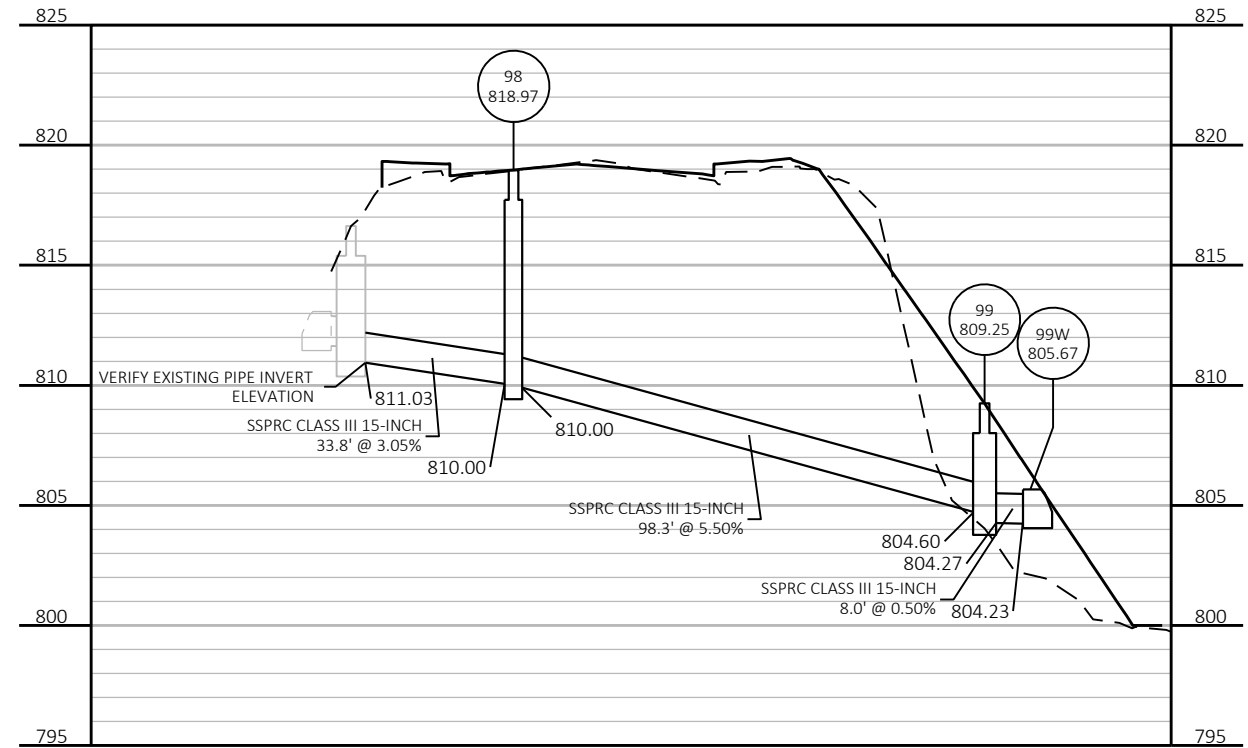
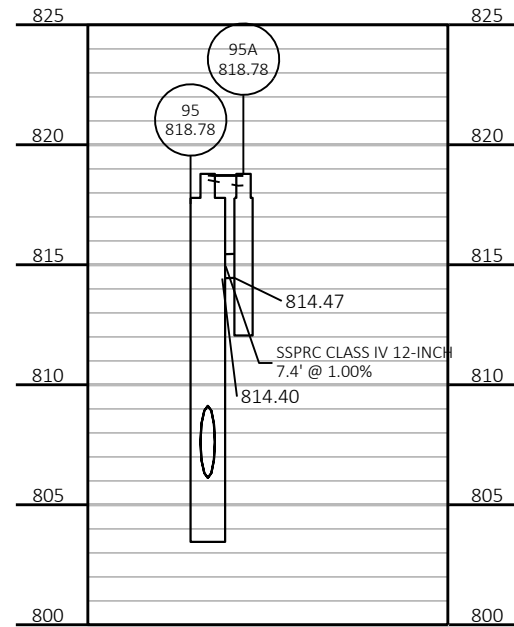
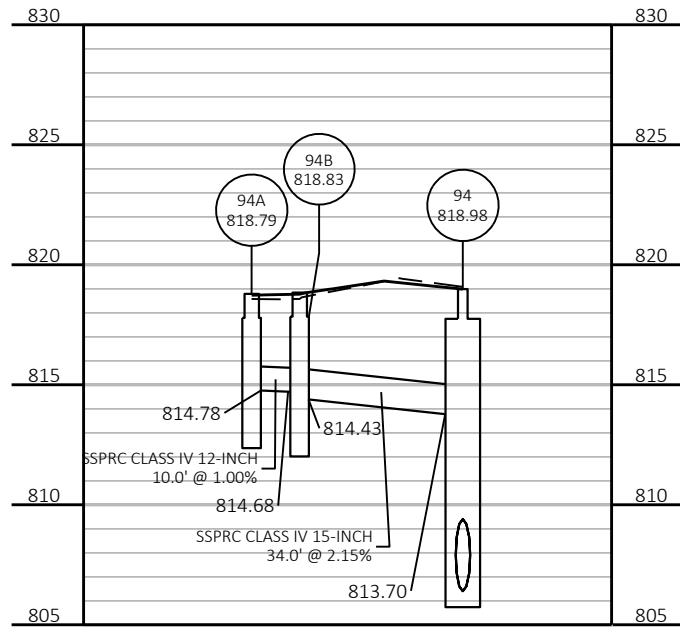
- LEGEND**
- RCB REMOVING CATCH BASIN
 - RPXX REMOVING STORM SEWER PIPE (DIAMETER AS NOTED)
 - RM REMOVING MANHOLES
 - ACB ABANDONING CATCH BASIN
 - AM ABANDONING MANHOLES
 - AS ABANDONING STORM SEWER





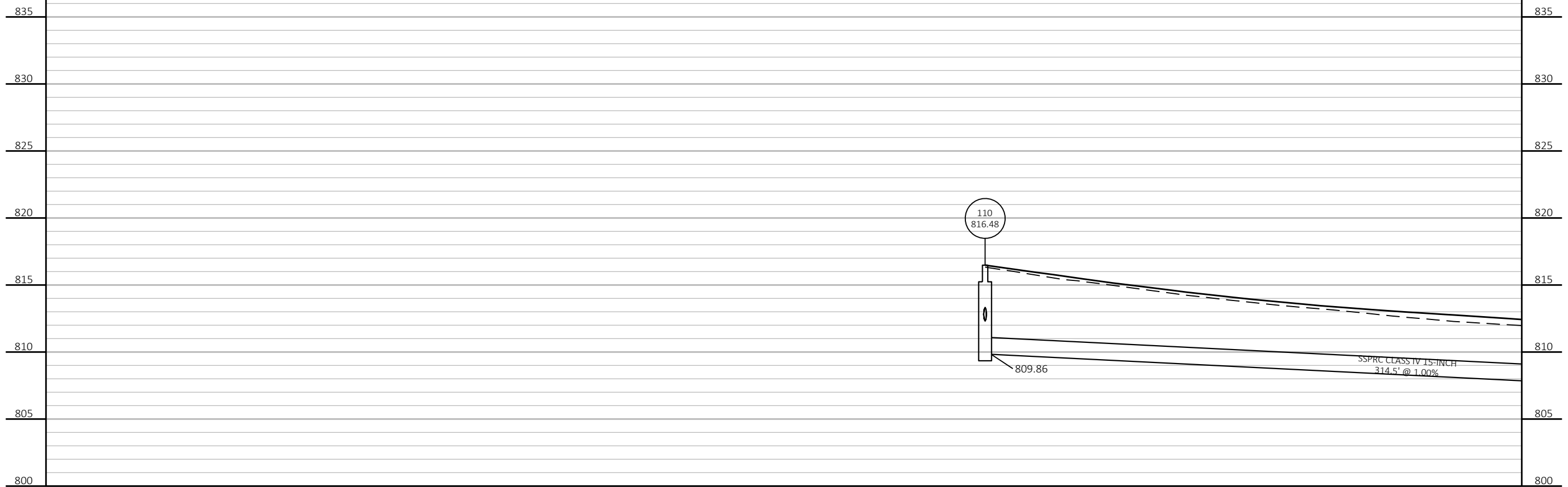
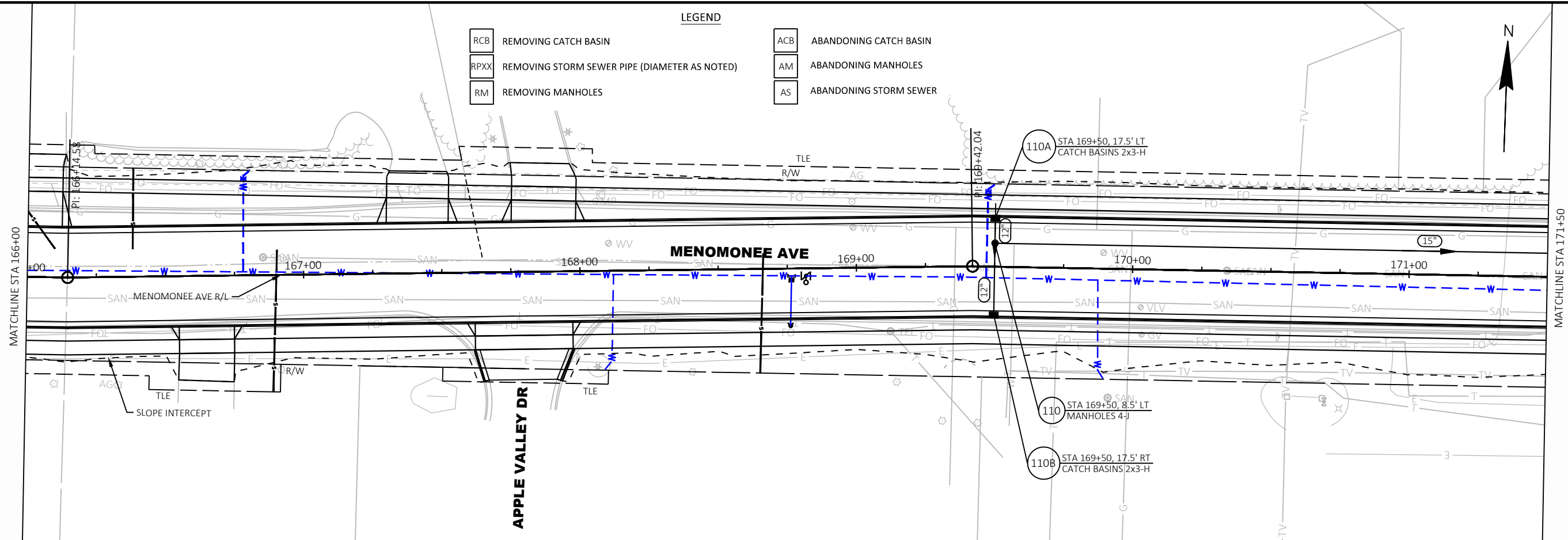


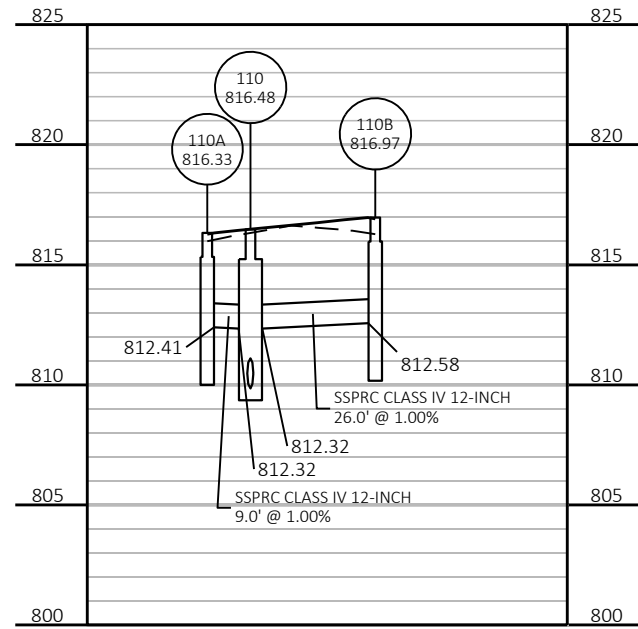
PROJECT NO: 2720-07-71	HWY: MENOMONEE AVE	COUNTY: WAUKESHA	STORM SEWER	SHEET	E
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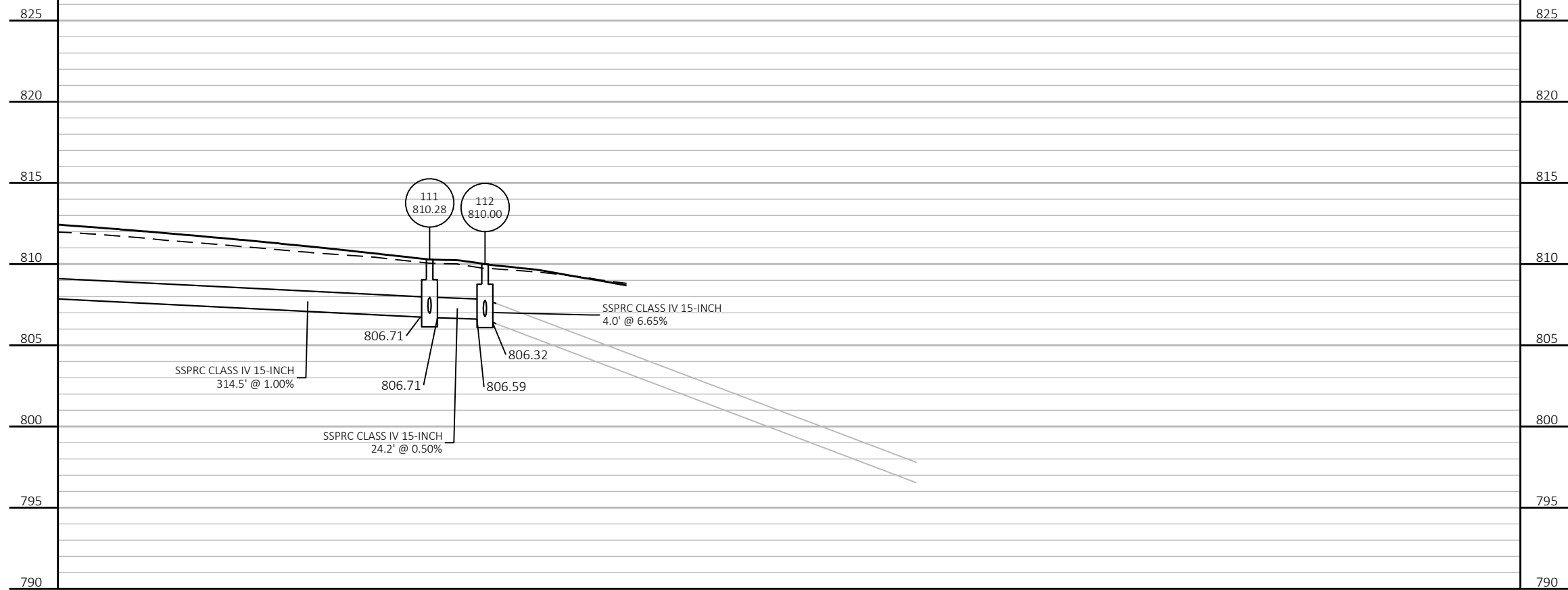
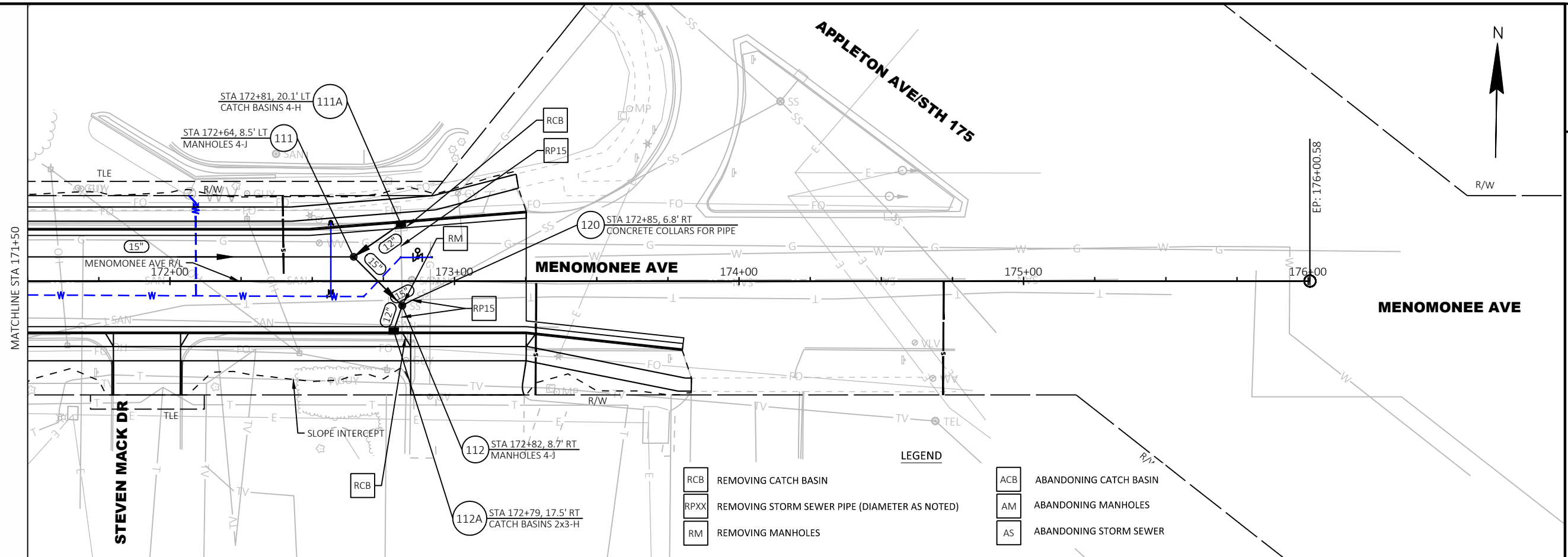


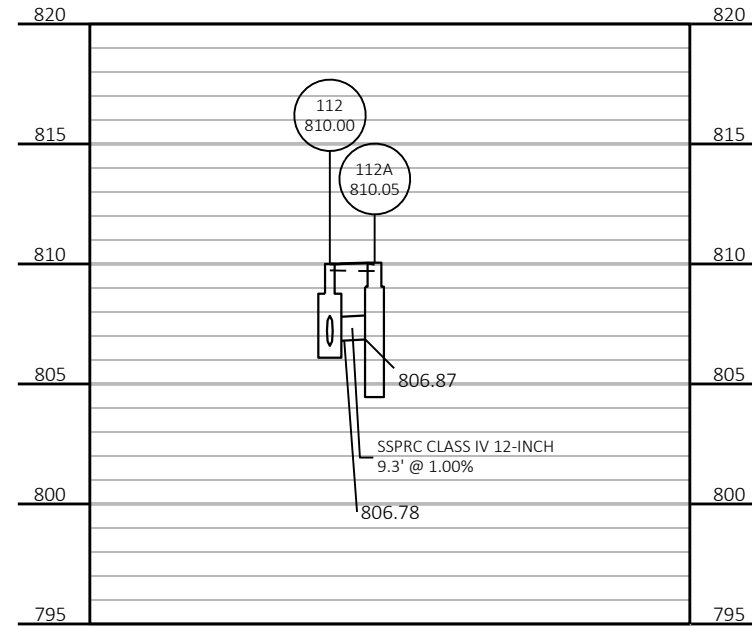
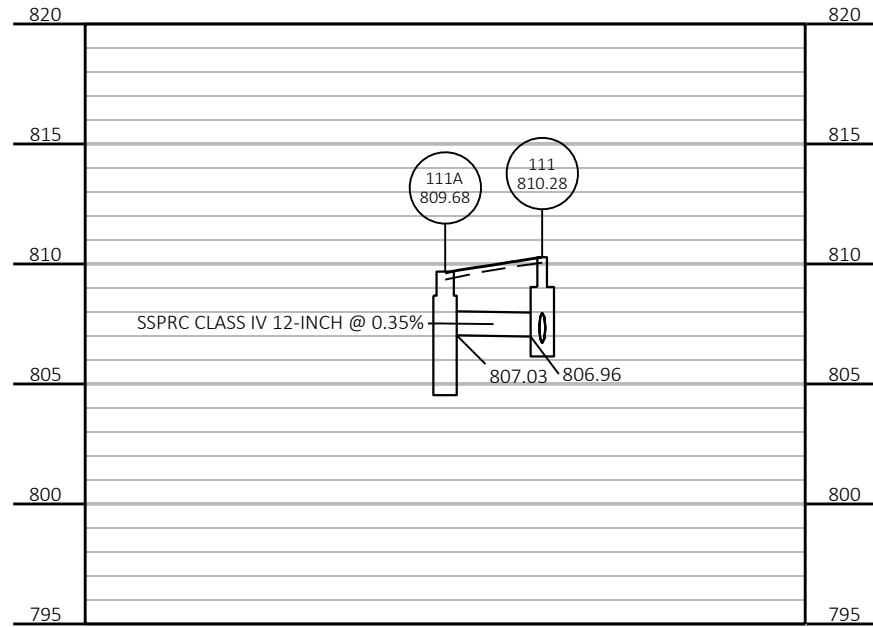
LEGEND

- RCB REMOVING CATCH BASIN
- RPXX REMOVING STORM SEWER PIPE (DIAMETER AS NOTED)
- RM REMOVING MANHOLES
- ACB ABANDONING CATCH BASIN
- AM ABANDONING MANHOLES
- AS ABANDONING STORM SEWER







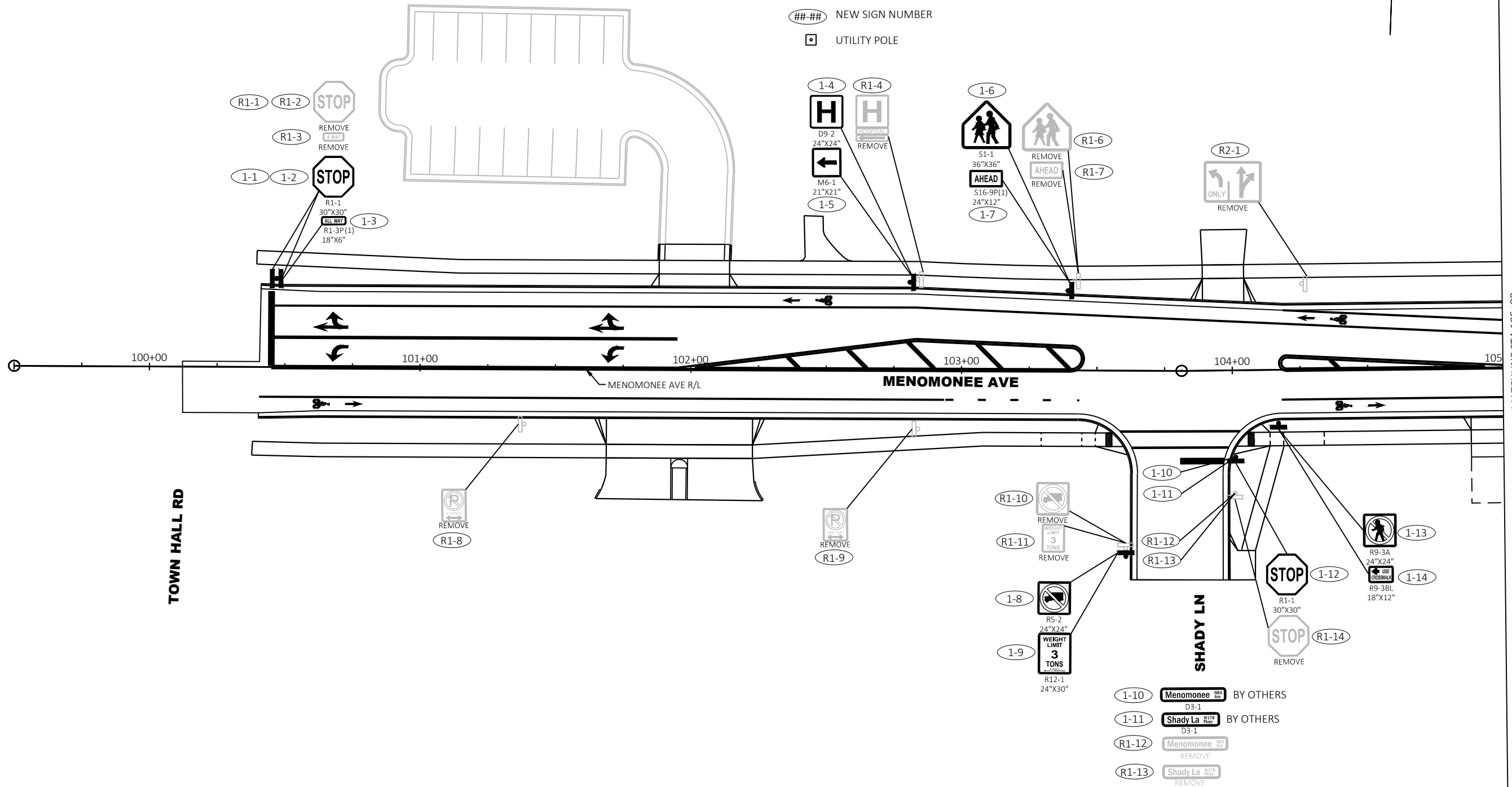


NOTES:

- 1. ALL STREET NAME SIGNS AND SUPPORTS SHALL BE SALVAGED AND DELIVERED TO VILLAGE OF MENOMONEE FALLS PUBLIC WORKS DEPARTMENT.
- 2. REMOVING AN EXISTING STOP SIGN SUPPORT AND ERECTING A NEW STOP SIGN/SUPPORT SHALL BE DONE CONCURRENTLY. FOR OTHER SIGNS/SUPPORTS THAT ARE TO BE REMOVED AND NEW SIGNS/SUPPORTS TO BE ERECTED, THE WORK SHOULD BE DONE AS CONCURRENTLY AS POSSIBLE.
- 3. ALL NEW SIGNS ARE SIZE 2S UNLESS OTHERWISE SPECIFIED.

LEGEND

- EXISTING SIGN MOUNTED ON POST(S)
- EXISTING SIGN MOUNTED ON LIGHT POLE, SIGNAL POLE, OR OVERHEAD SUPPORT
- SIGN NUMBER (REMOVING SIGNS TYPE II)
- PROPOSED SIGN MOUNTED ON WOOD OR TUBULAR STEEL POST(S)
- PROPOSED SIGN MOUNTED ON LIGHT POLE, SIGNAL POLE, OR OVERHEAD SUPPORT
- NEW SIGN NUMBER
- UTILITY POLE



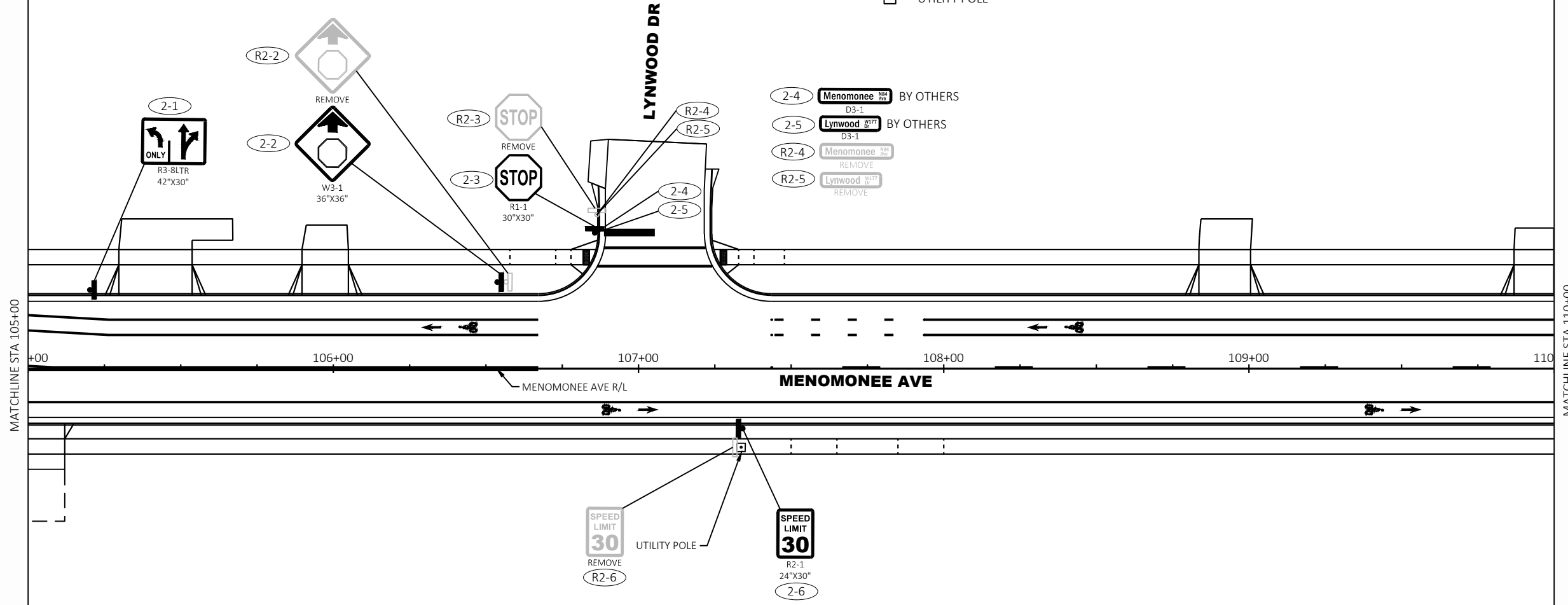
MATCHLINE STA 105+00

NOTES:

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LEGEND

- EXISTING SIGN MOUNTED ON POST(S)
- EXISTING SIGN MOUNTED ON LIGHT POLE, SIGNAL POLE, OR OVERHEAD SUPPORT
- SIGN NUMBER (REMOVING SIGNS TYPE II)
- PROPOSED SIGN MOUNTED ON WOOD OR TUBULAR STEEL POST(S)
- PROPOSED SIGN MOUNTED ON LIGHT POLE, SIGNAL POLE, OR OVERHEAD SUPPORT
- NEW SIGN NUMBER
- UTILITY POLE



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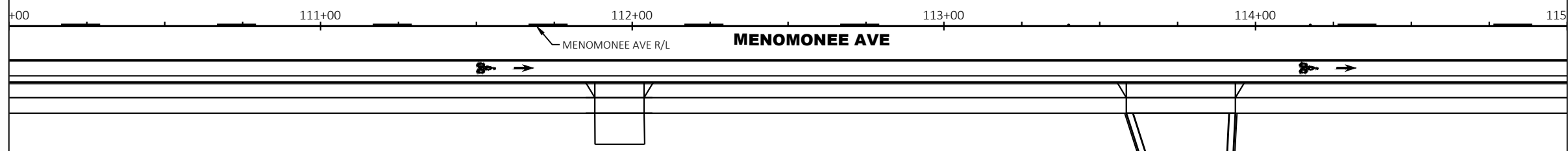
LEGEND

- EXISTING SIGN MOUNTED ON POST(S)
- EXISTING SIGN MOUNTED ON LIGHT POLE, SIGNAL POLE, OR OVERHEAD SUPPORT
- SIGN NUMBER (REMOVING SIGNS TYPE II)
- PROPOSED SIGN MOUNTED ON WOOD OR TUBULAR STEEL POST(S)
- PROPOSED SIGN MOUNTED ON LIGHT POLE, SIGNAL POLE, OR OVERHEAD SUPPORT
- NEW SIGN NUMBER
- UTILITY POLE



MATCHLINE STA 110+00

MATCHLINE STA 115+00



- 3-3 **Menomonee Ave** BY OTHERS
D3-1
- 3-4 **Sunset Ridge Ave** BY OTHERS
D3-1
- R3-3 **Menomonee Ave**
REMOVE
- R3-4 **Sunset Ridge Ave**
REMOVE

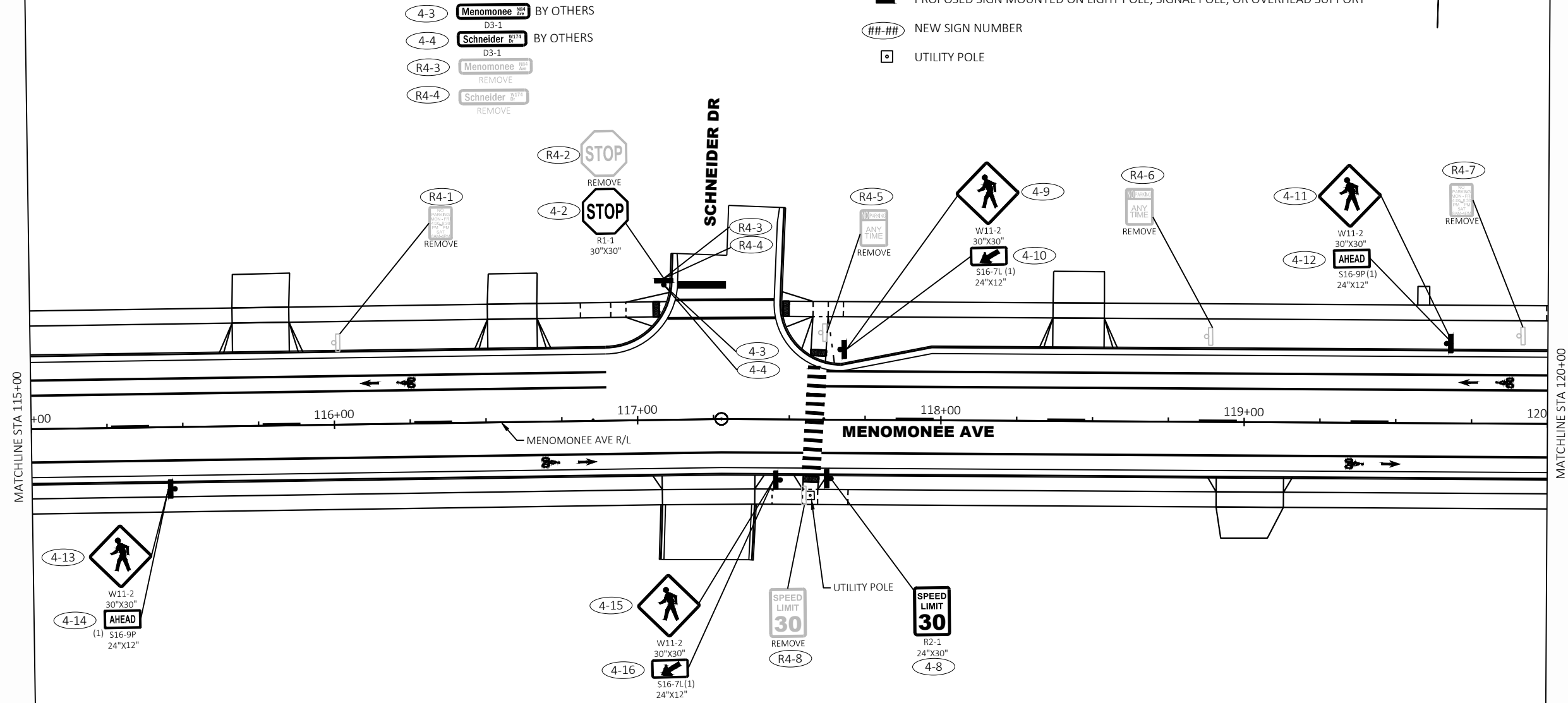
NOTES:

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2. REMOVING AN EXISTING STOP SIGN SUPPORT AND ERECTING A NEW STOP SIGN/SUPPORT SHALL BE DONE CONCURRENTLY. FOR OTHER SIGNS/SUPPORTS THAT ARE TO BE REMOVED AND NEW SIGNS/SUPPORTS TO BE ERECTED, THE WORK SHOULD BE DONE AS CONCURRENTLY AS POSSIBLE.
3. ALL NEW SIGNS ARE SIZE 2S UNLESS OTHERWISE SPECIFIED.

- 4-3 **Menomonee** D3-1 BY OTHERS
- 4-4 **Schneider** D3-1 BY OTHERS
- R4-3 **Menomonee** REMOVE
- R4-4 **Schneider** REMOVE

LEGEND

- EXISTING SIGN MOUNTED ON POST(S)
- EXISTING SIGN MOUNTED ON LIGHT POLE, SIGNAL POLE, OR OVERHEAD SUPPORT
- SIGN NUMBER (REMOVING SIGNS TYPE II)
- PROPOSED SIGN MOUNTED ON WOOD OR TUBULAR STEEL POST(S)
- PROPOSED SIGN MOUNTED ON LIGHT POLE, SIGNAL POLE, OR OVERHEAD SUPPORT
- NEW SIGN NUMBER
- UTILITY POLE



NOTES:

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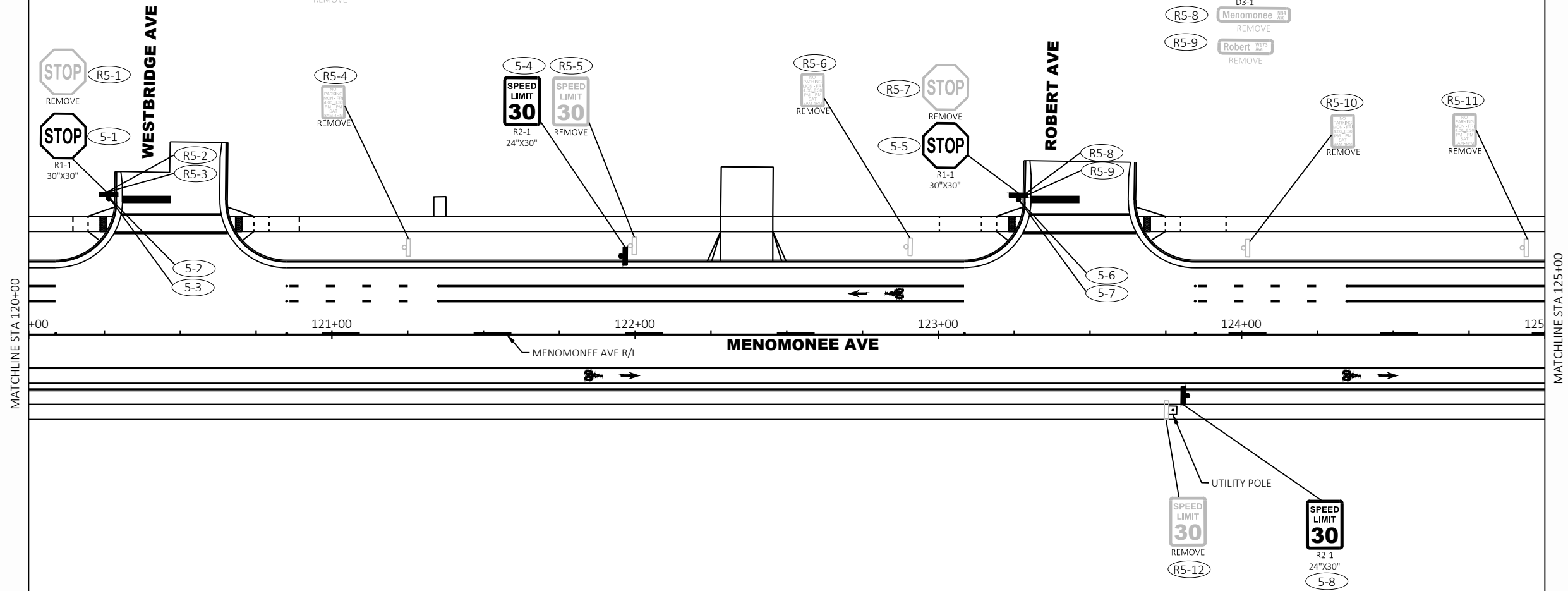
LEGEND

- EXISTING SIGN MOUNTED ON POST(S)
- EXISTING SIGN MOUNTED ON LIGHT POLE, SIGNAL POLE, OR OVERHEAD SUPPORT
- SIGN NUMBER (REMOVING SIGNS TYPE II)
- PROPOSED SIGN MOUNTED ON WOOD OR TUBULAR STEEL POST(S)
- PROPOSED SIGN MOUNTED ON LIGHT POLE, SIGNAL POLE, OR OVERHEAD SUPPORT
- NEW SIGN NUMBER
- UTILITY POLE



- 5-2 **Menomonee** BY OTHERS
D3-1
- 5-3 **Westbridge** BY OTHERS
D3-1
- R5-2 **Menomonee** REMOVE
- R5-3 **Westbridge** REMOVE

- 5-6 **Menomonee** BY OTHERS
D3-1
- 5-7 **Robert** BY OTHERS
D3-1
- R5-8 **Menomonee** REMOVE
- R5-9 **Robert** REMOVE



NOTES:

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- 6-2 **Menomonee** BY OTHERS
D3-1
- 6-3 **Shady** BY OTHERS
D3-1
- R6-2 **Menomonee** REMOVE
- R6-3 **Shady** REMOVE

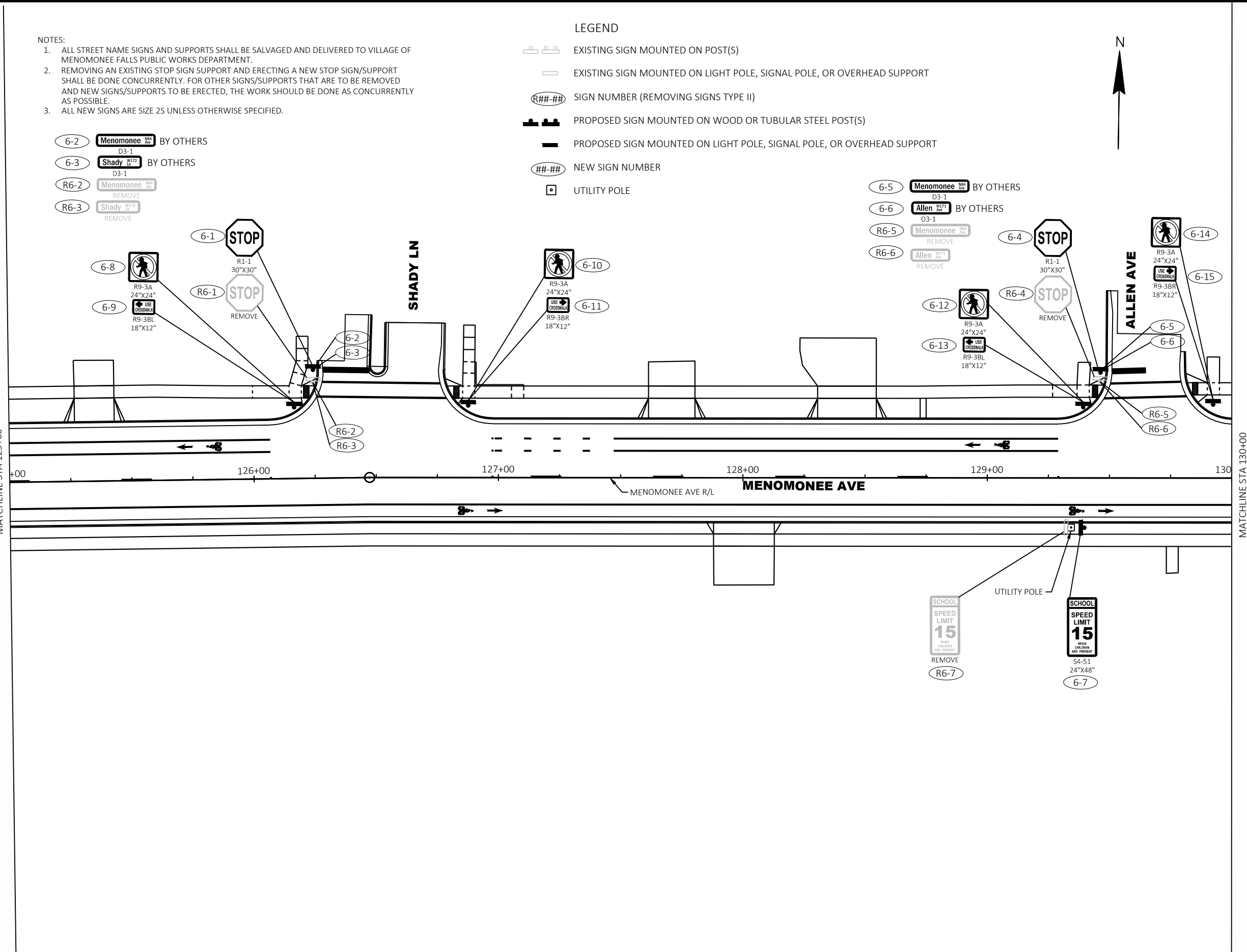
LEGEND

- EXISTING SIGN MOUNTED ON POST(S)
- EXISTING SIGN MOUNTED ON LIGHT POLE, SIGNAL POLE, OR OVERHEAD SUPPORT
- SIGN NUMBER (REMOVING SIGNS TYPE II)
- PROPOSED SIGN MOUNTED ON WOOD OR TUBULAR STEEL POST(S)
- PROPOSED SIGN MOUNTED ON LIGHT POLE, SIGNAL POLE, OR OVERHEAD SUPPORT
- NEW SIGN NUMBER
- UTILITY POLE

- 6-5 **Menomonee** BY OTHERS
D3-1
- 6-6 **Allen** BY OTHERS
D3-1
- R6-5 **Menomonee** REMOVE
- R6-6 **Allen** REMOVE

MATCHLINE STA 125+00

MATCHLINE STA 130+00

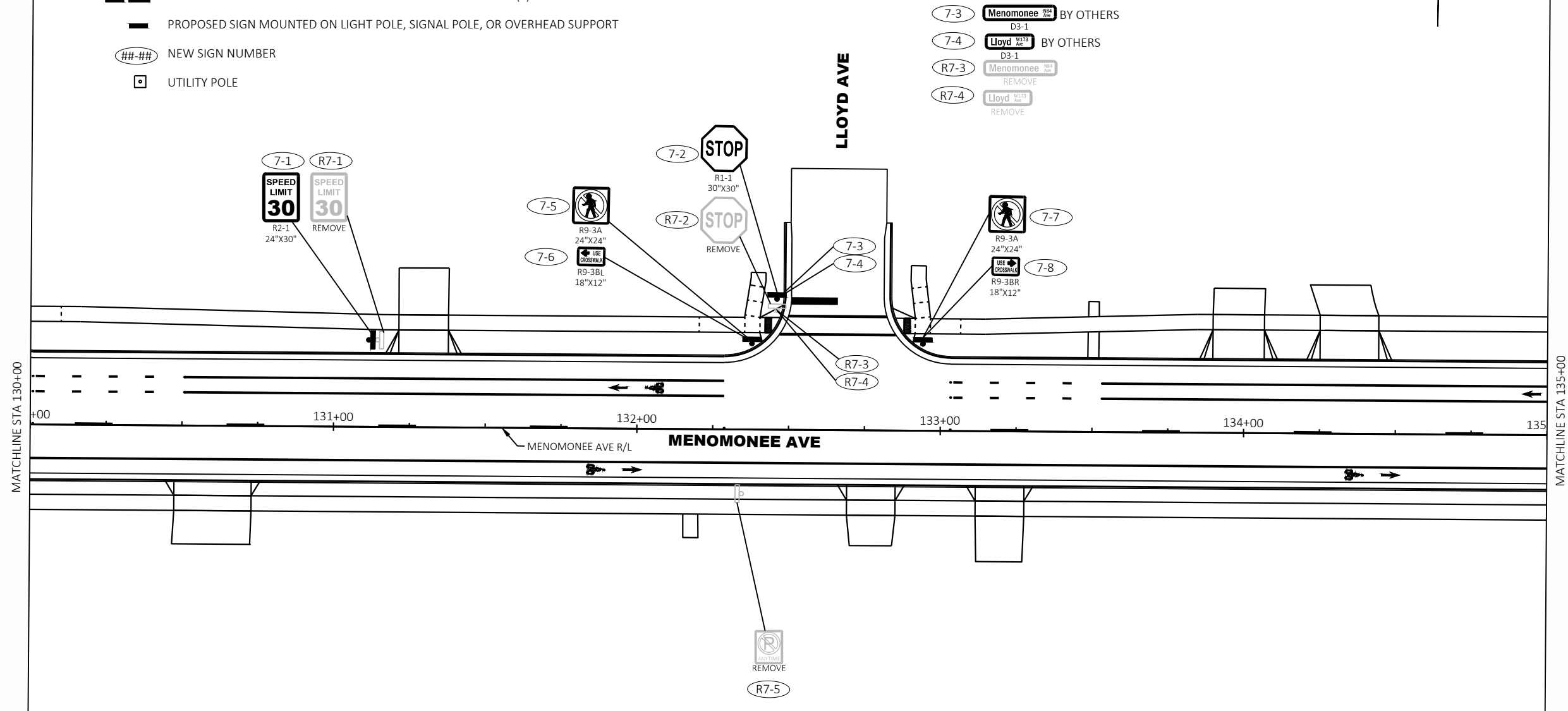


LEGEND

- EXISTING SIGN MOUNTED ON POST(S)
- EXISTING SIGN MOUNTED ON LIGHT POLE, SIGNAL POLE, OR OVERHEAD SUPPORT
- SIGN NUMBER (REMOVING SIGNS TYPE II)
- PROPOSED SIGN MOUNTED ON WOOD OR TUBULAR STEEL POST(S)
- PROPOSED SIGN MOUNTED ON LIGHT POLE, SIGNAL POLE, OR OVERHEAD SUPPORT
- NEW SIGN NUMBER
- UTILITY POLE

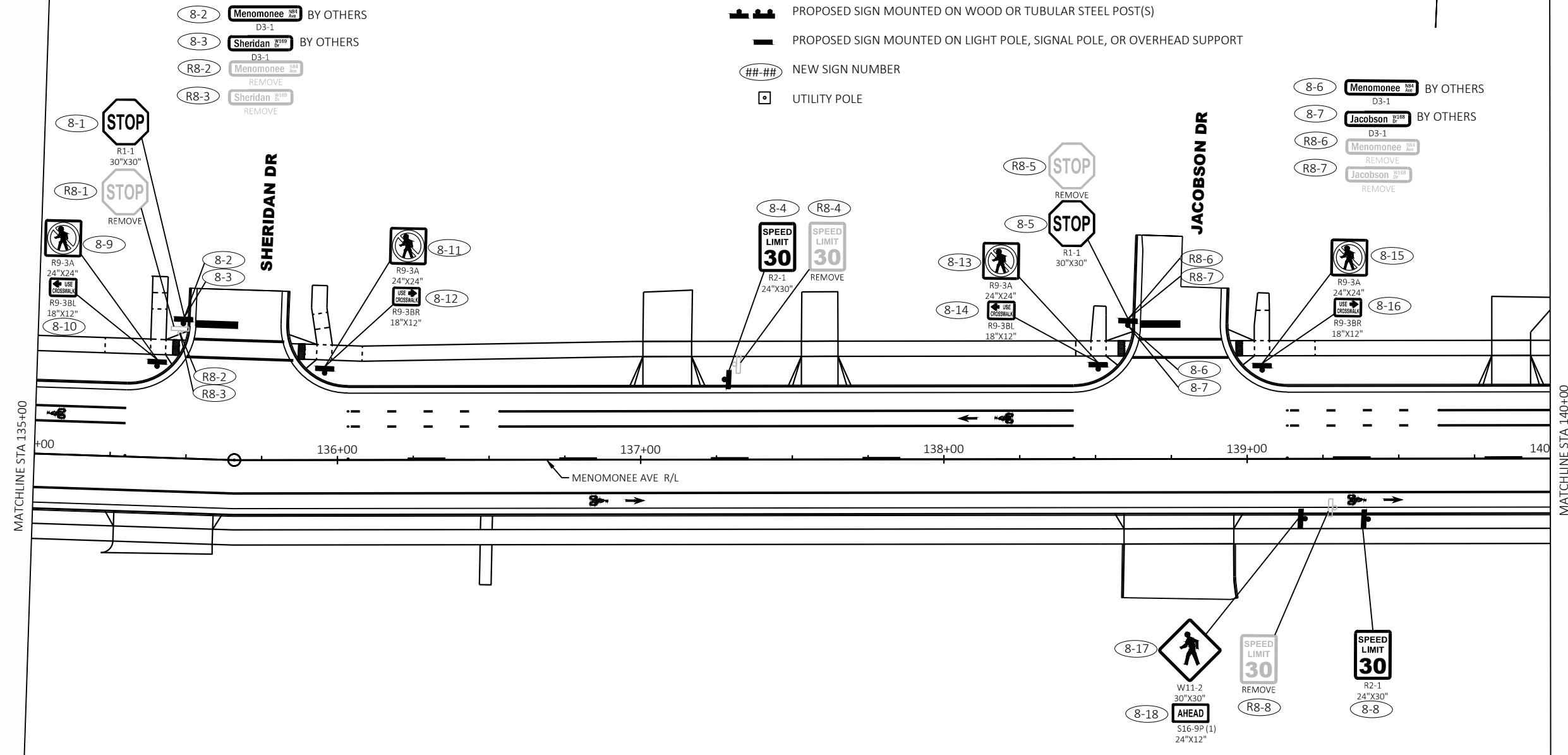
NOTES:

1. ALL STREET NAME SIGNS AND SUPPORTS SHALL BE SALVAGED AND DELIVERED TO VILLAGE OF MEMOMONEE FALLS PUBLIC WORKS DEPARTMENT.
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- NOTES:
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 3. ALL NEW SIGNS ARE SIZE 2S UNLESS OTHERWISE SPECIFIED.

- LEGEND
- EXISTING SIGN MOUNTED ON POST(S)
 - EXISTING SIGN MOUNTED ON LIGHT POLE, SIGNAL POLE, OR OVERHEAD SUPPORT
 - SIGN NUMBER (REMOVING SIGNS TYPE II)
 - PROPOSED SIGN MOUNTED ON WOOD OR TUBULAR STEEL POST(S)
 - PROPOSED SIGN MOUNTED ON LIGHT POLE, SIGNAL POLE, OR OVERHEAD SUPPORT
 - NEW SIGN NUMBER
 - UTILITY POLE



- 8-2 **Menomonee** BY OTHERS
D3-1
- 8-3 **Sheridan** BY OTHERS
D3-1
- R8-2 **Menomonee** REMOVE
- R8-3 **Sheridan** REMOVE

- 8-6 **Menomonee** BY OTHERS
D3-1
- 8-7 **Jacobson** BY OTHERS
D3-1
- R8-6 **Menomonee** REMOVE
- R8-7 **Jacobson** REMOVE

MATCHLINE STA 135+00

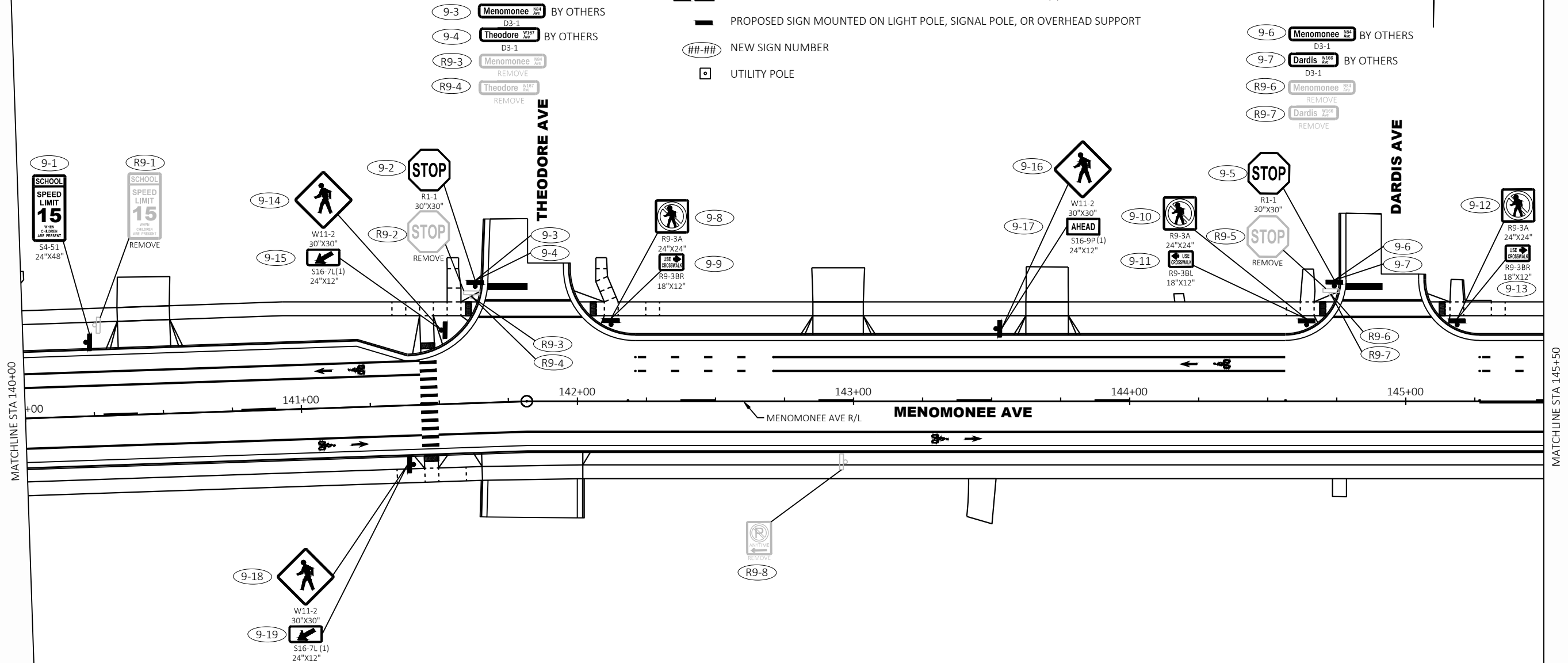
MATCHLINE STA 140+00

NOTES:

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LEGEND

- EXISTING SIGN MOUNTED ON POST(S)
- EXISTING SIGN MOUNTED ON LIGHT POLE, SIGNAL POLE, OR OVERHEAD SUPPORT
- SIGN NUMBER (REMOVING SIGNS TYPE II)
- PROPOSED SIGN MOUNTED ON WOOD OR TUBULAR STEEL POST(S)
- PROPOSED SIGN MOUNTED ON LIGHT POLE, SIGNAL POLE, OR OVERHEAD SUPPORT
- NEW SIGN NUMBER
- UTILITY POLE

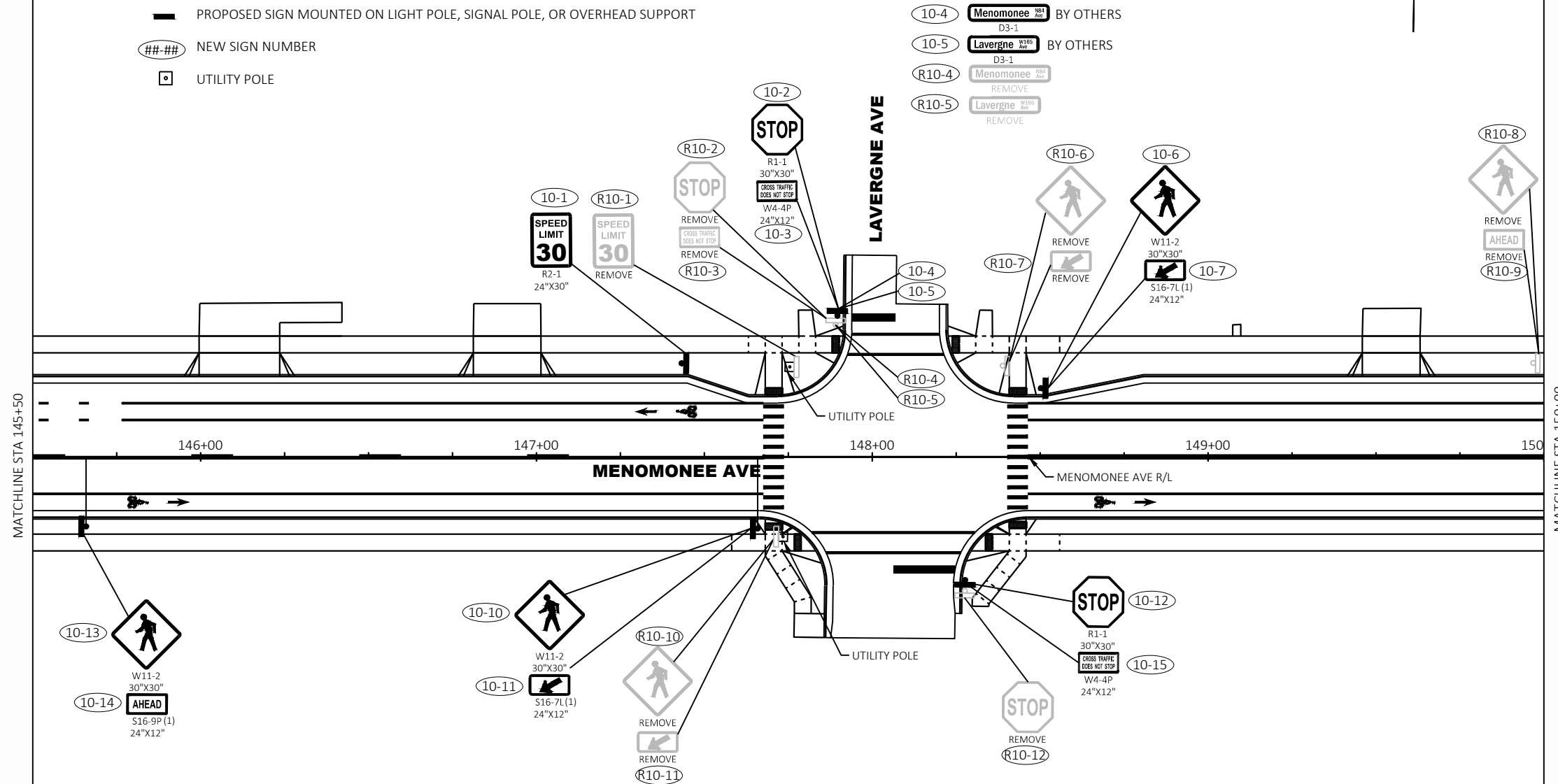


LEGEND

- EXISTING SIGN MOUNTED ON POST(S)
- EXISTING SIGN MOUNTED ON LIGHT POLE, SIGNAL POLE, OR OVERHEAD SUPPORT
- SIGN NUMBER (REMOVING SIGNS TYPE II)
- PROPOSED SIGN MOUNTED ON WOOD OR TUBULAR STEEL POST(S)
- PROPOSED SIGN MOUNTED ON LIGHT POLE, SIGNAL POLE, OR OVERHEAD SUPPORT
- NEW SIGN NUMBER
- UTILITY POLE

NOTES:

1. ALL STREET NAME SIGNS AND SUPPORTS SHALL BE SALVAGED AND DELIVERED TO VILLAGE OF MENOMONEE FALLS PUBLIC WORKS DEPARTMENT.
2. REMOVING AN EXISTING STOP SIGN SUPPORT AND ERECTING A NEW STOP SIGN/SUPPORT SHALL BE DONE CONCURRENTLY. FOR OTHER SIGNS/SUPPORTS THAT ARE TO BE REMOVED AND NEW SIGNS/SUPPORTS TO BE ERECTED, THE WORK SHOULD BE DONE AS CONCURRENTLY AS POSSIBLE.
3. ALL NEW SIGNS ARE SIZE 2S UNLESS OTHERWISE SPECIFIED.

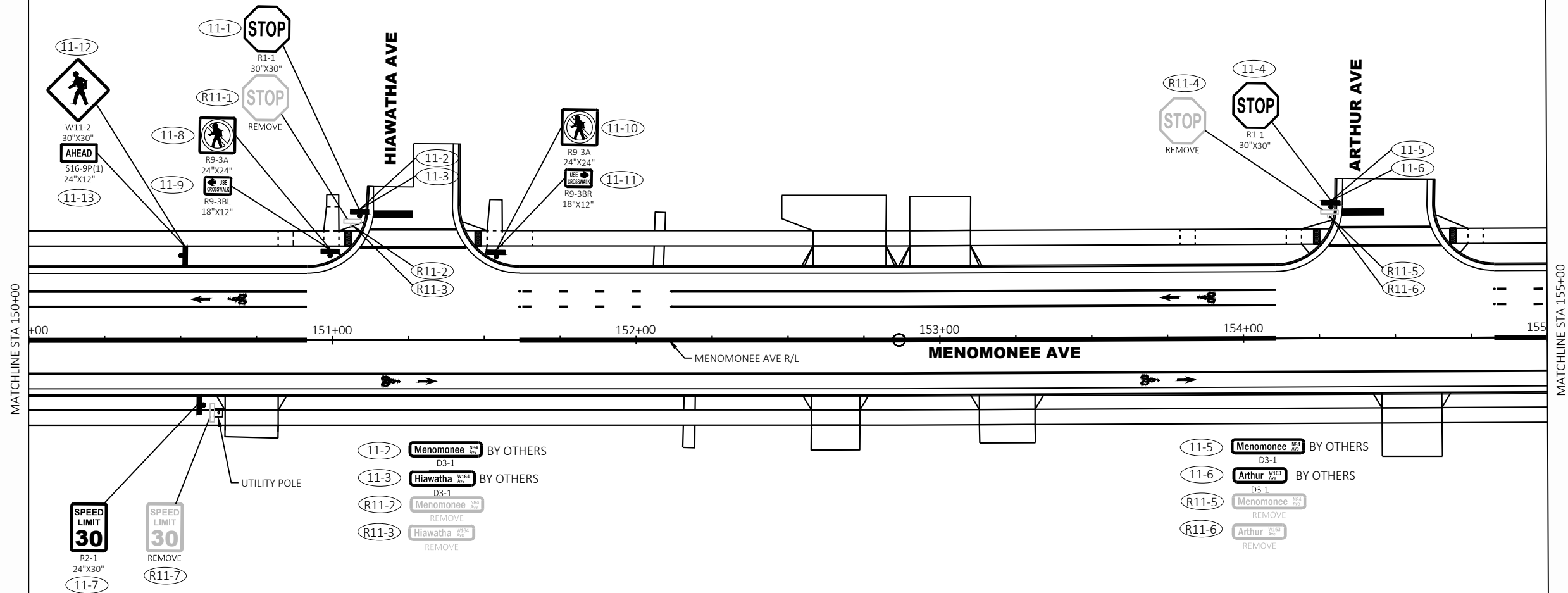


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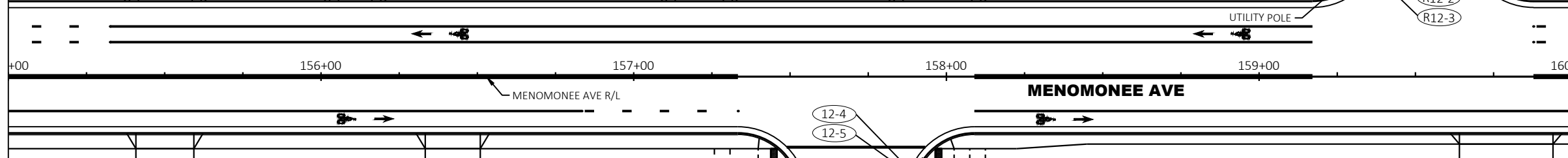
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- NEW SIGN NUMBER
- UTILITY POLE



MATCHLINE STA 155+00

MATCHLINE STA 160+00



- 12-2 BY OTHERS D3-1
- 12-3 BY OTHERS D3-1
- R12-2 REMOVE
- R12-3 REMOVE








- 12-1 R1-1 30"X30"
- R12-1 REMOVE
- 12-7 R9-3A 24"X24"
- 12-8 R9-3BL 18"X12"
- 12-9 R9-3A 24"X24"
- R12-9 R9-3BR 18"X12"
- 12-10 R9-3BL 18"X12"
- R12-2 REMOVE
- R12-3 REMOVE

- 12-4 BY OTHERS D3-1
- 12-5 BY OTHERS D3-1
- R12-4 REMOVE
- R12-5 REMOVE
- R12-6 REMOVE
- 12-6 R1-1 30"X30"

- 12-4 BY OTHERS D3-1
- 12-5 BY OTHERS D3-1
- R12-4 REMOVE
- R12-5 REMOVE

- NOTES:
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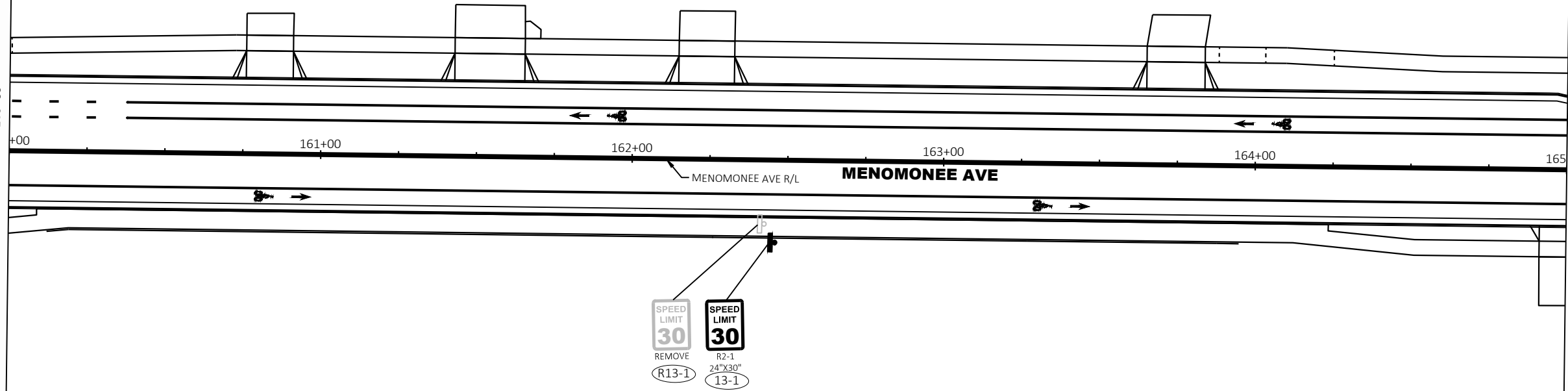
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-  PROPOSED SIGN MOUNTED ON LIGHT POLE, SIGNAL POLE, OR OVERHEAD SUPPORT
-  NEW SIGN NUMBER
-  UTILITY POLE



MATCHLINE STA 160+00








MATCHLINE STA 165+00

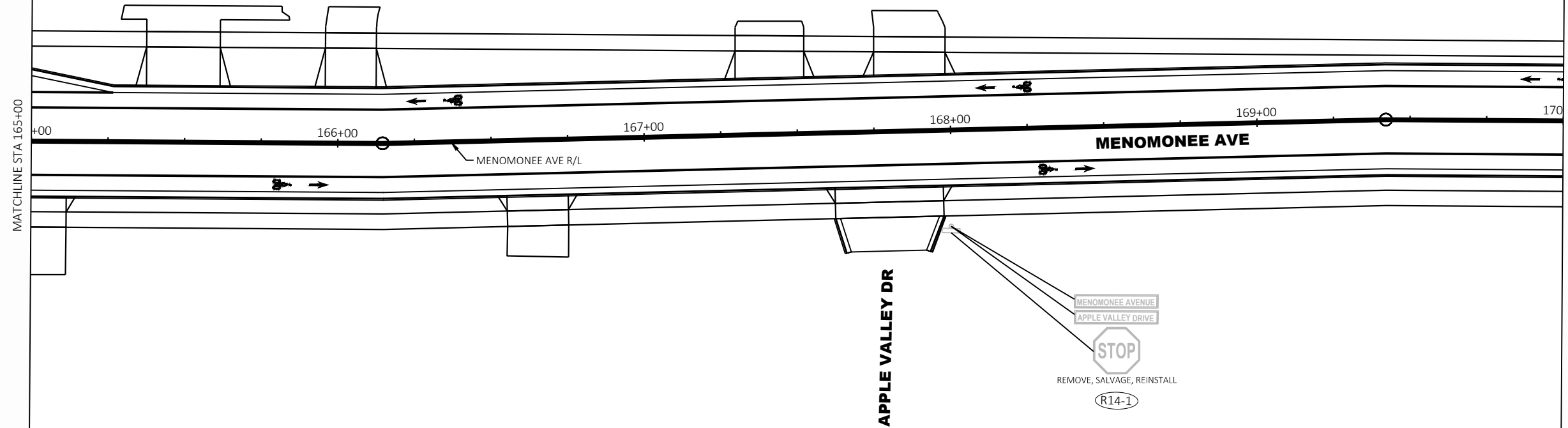


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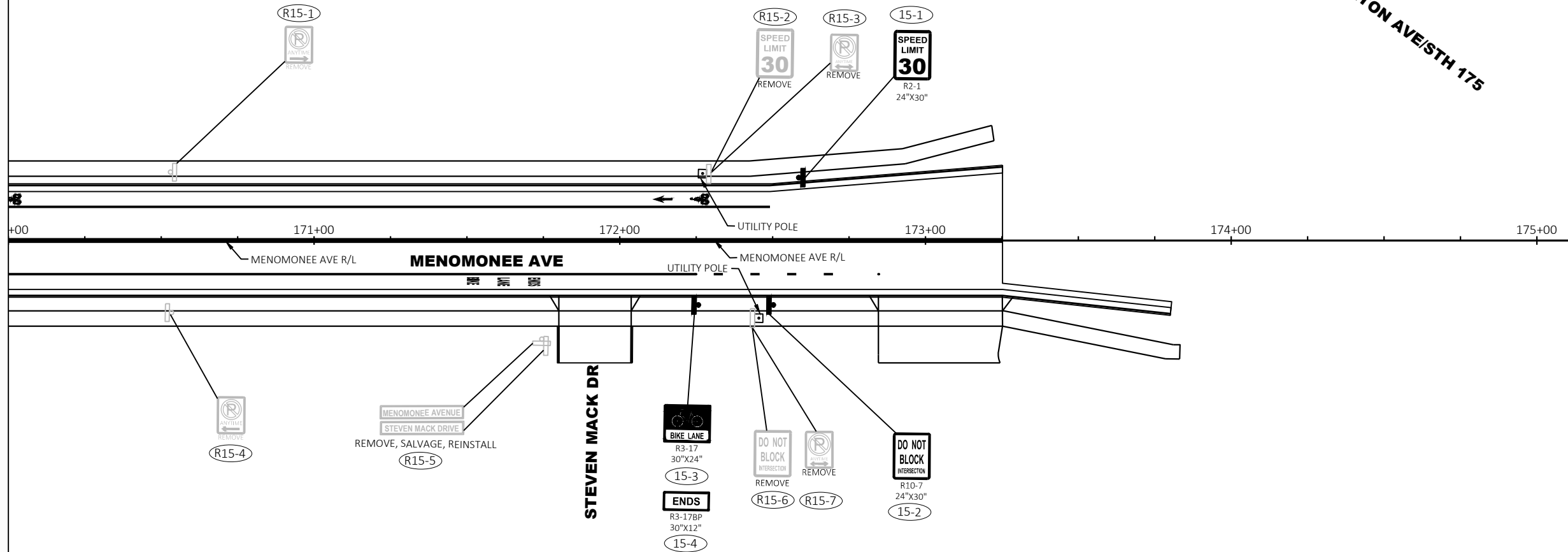
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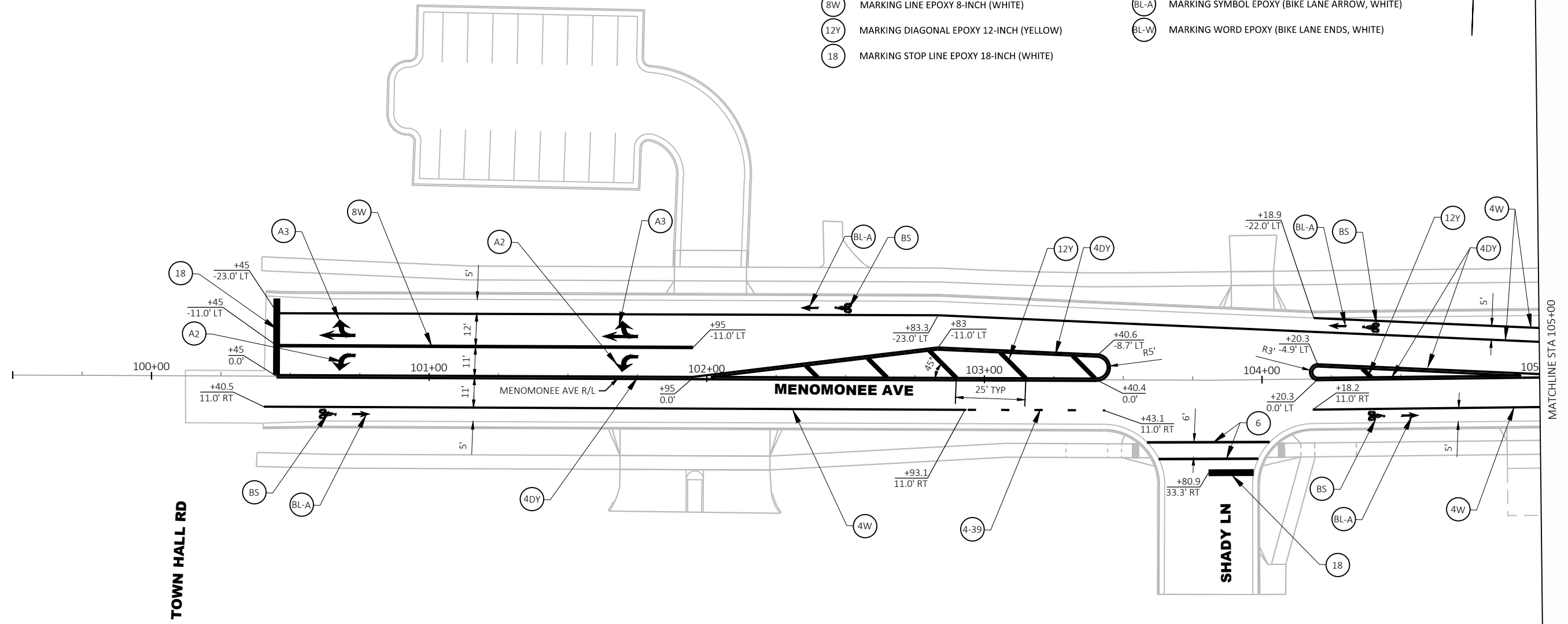


MATCHLINE STA. 170+00



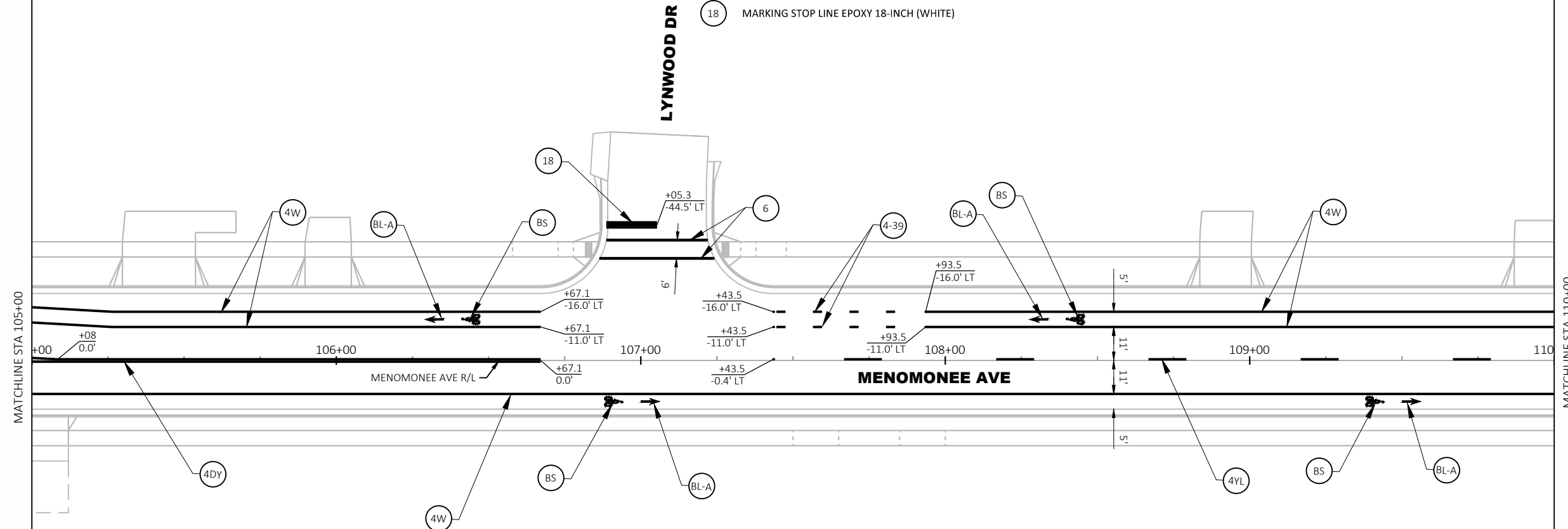
PAVEMENT MARKING LEGEND

- (4Y) MARKING LINE EPOXY 4-INCH (YELLOW)
- (4DY) MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW)
- (4YL) MARKING LINE EPOXY 4-INCH (12.5' LINE, 37.5' GAP, YELLOW)
- (4W) MARKING LINE EPOXY 4-INCH (WHITE)
- (4-39) MARKING LINE EPOXY 4-INCH (3' LINE, 9' GAP, WHITE)
- (8W) MARKING LINE EPOXY 8-INCH (WHITE)
- (12Y) MARKING DIAGONAL EPOXY 12-INCH (YELLOW)
- (18) MARKING STOP LINE EPOXY 18-INCH (WHITE)
- (6) MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)
- (24B) MARKING CROSSWALK EPOXY BLOCK STYLE 24-INCH (WHITE)
- (A2) MARKING ARROW EPOXY (TYPE 2, WHITE)
- (A3) MARKING ARROW EPOXY (TYPE 3, WHITE)
- (BS) MARKING SYMBOL EPOXY (BIKE LANE, WHITE)
- (BL-A) MARKING SYMBOL EPOXY (BIKE LANE ARROW, WHITE)
- (BL-W) MARKING WORD EPOXY (BIKE LANE ENDS, WHITE)



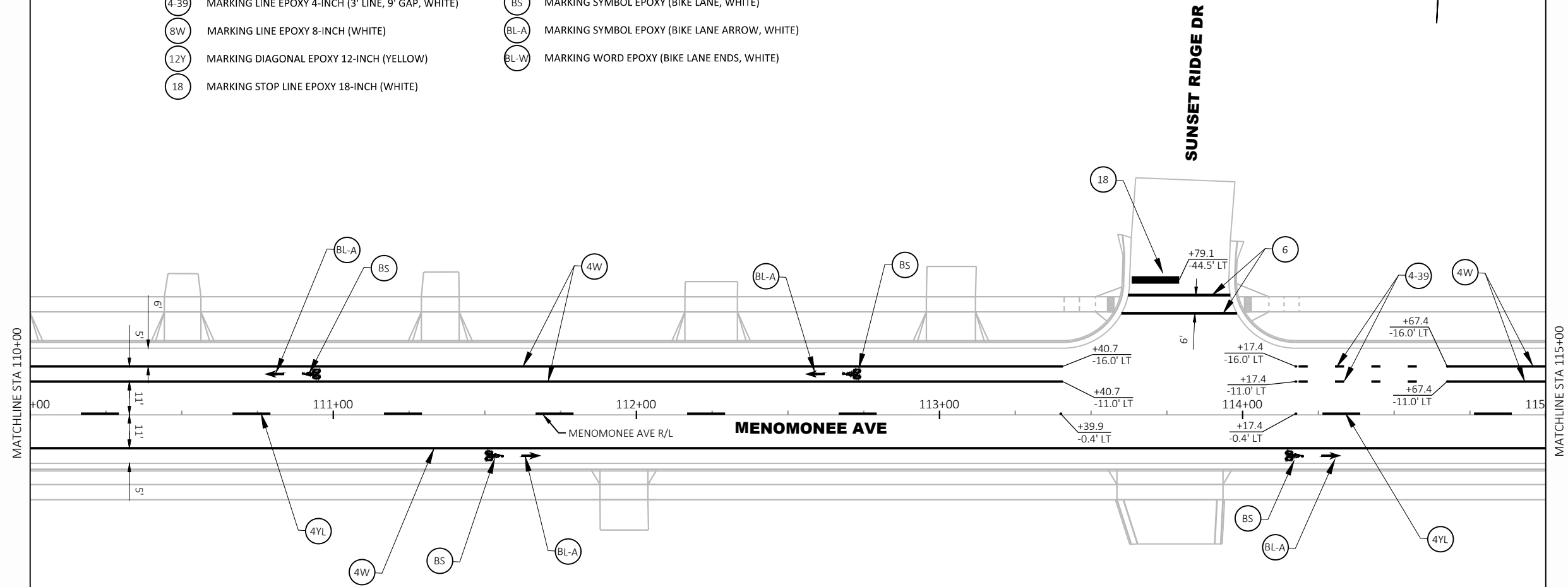
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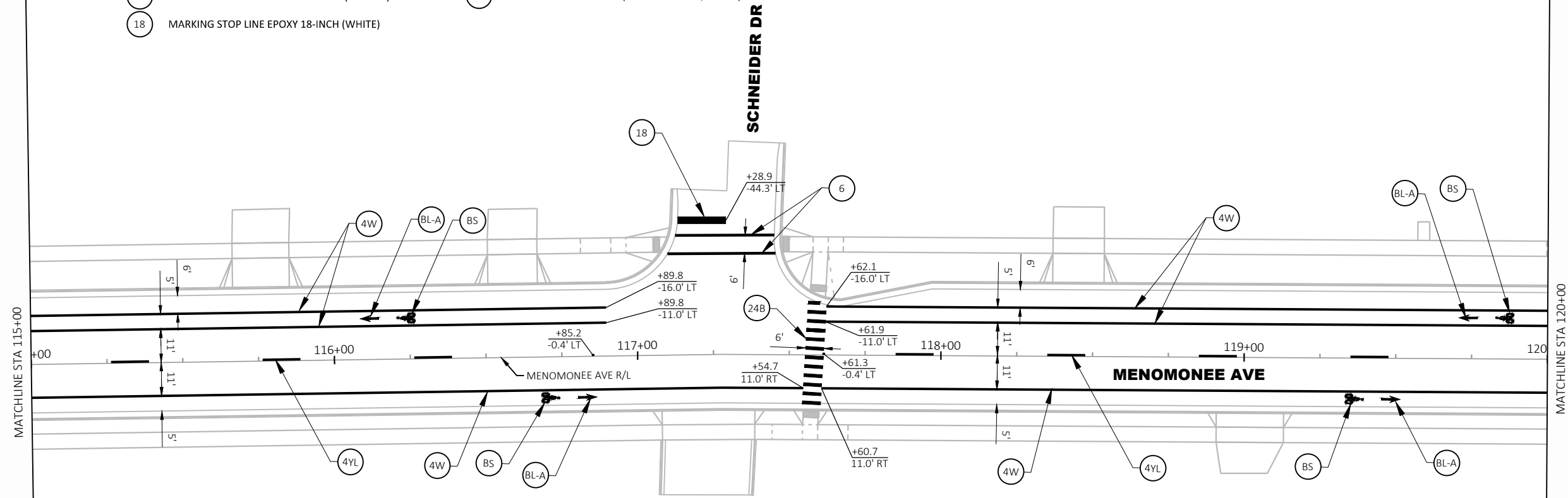
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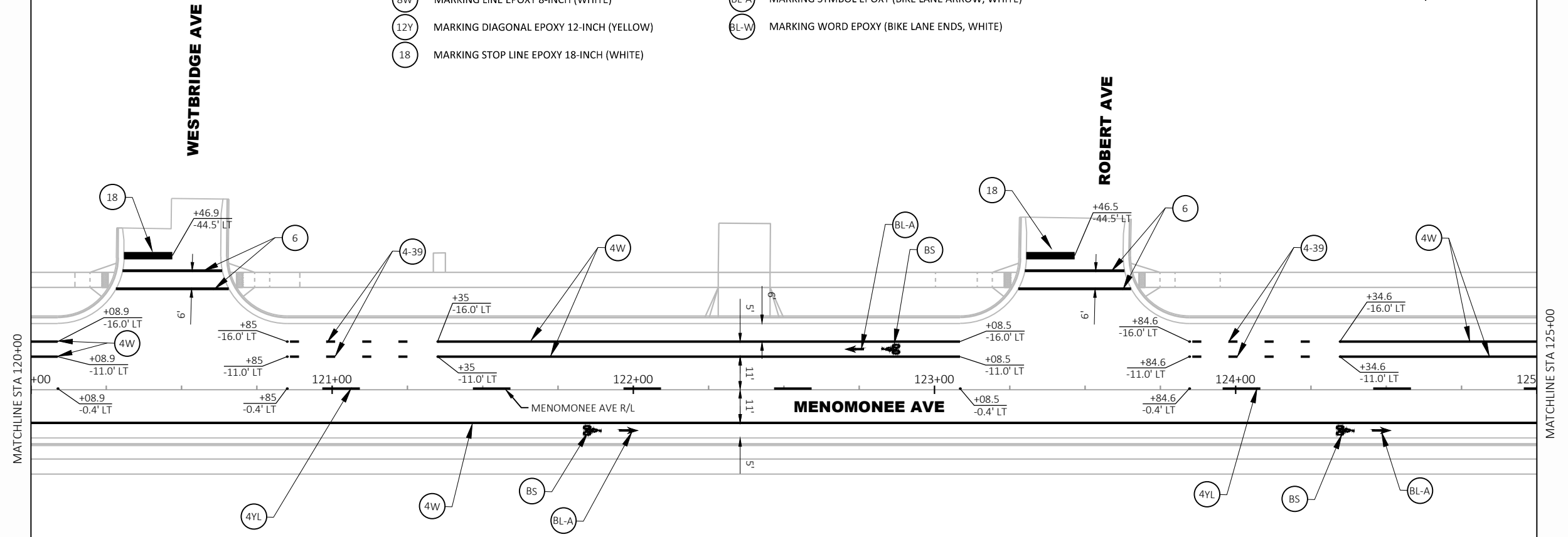
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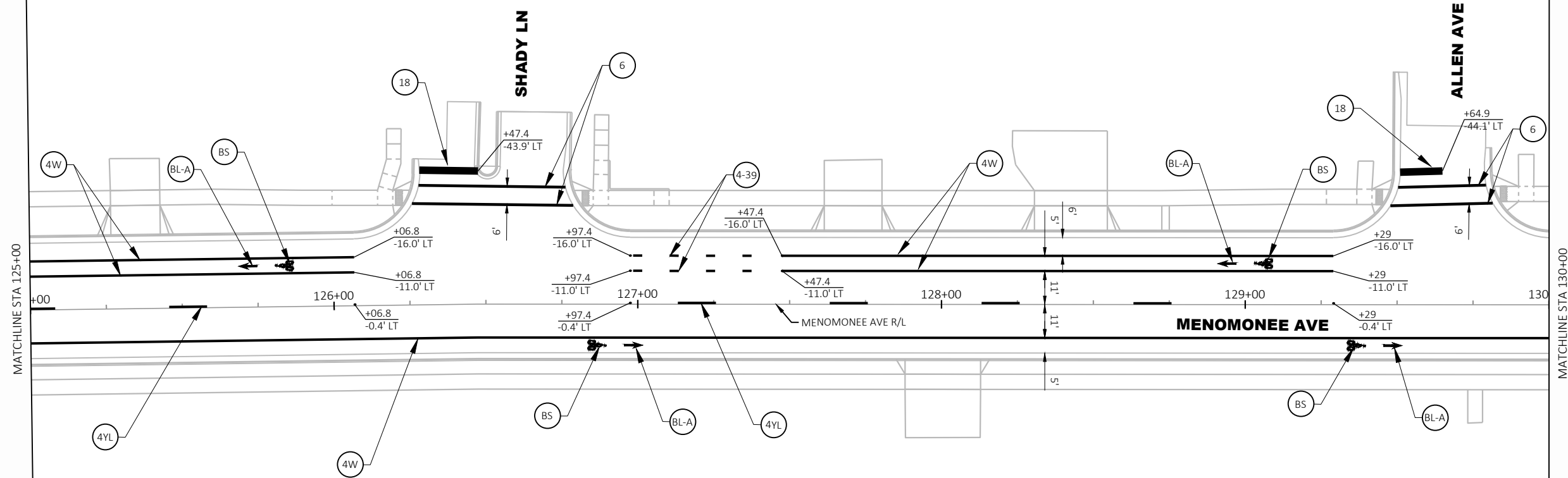
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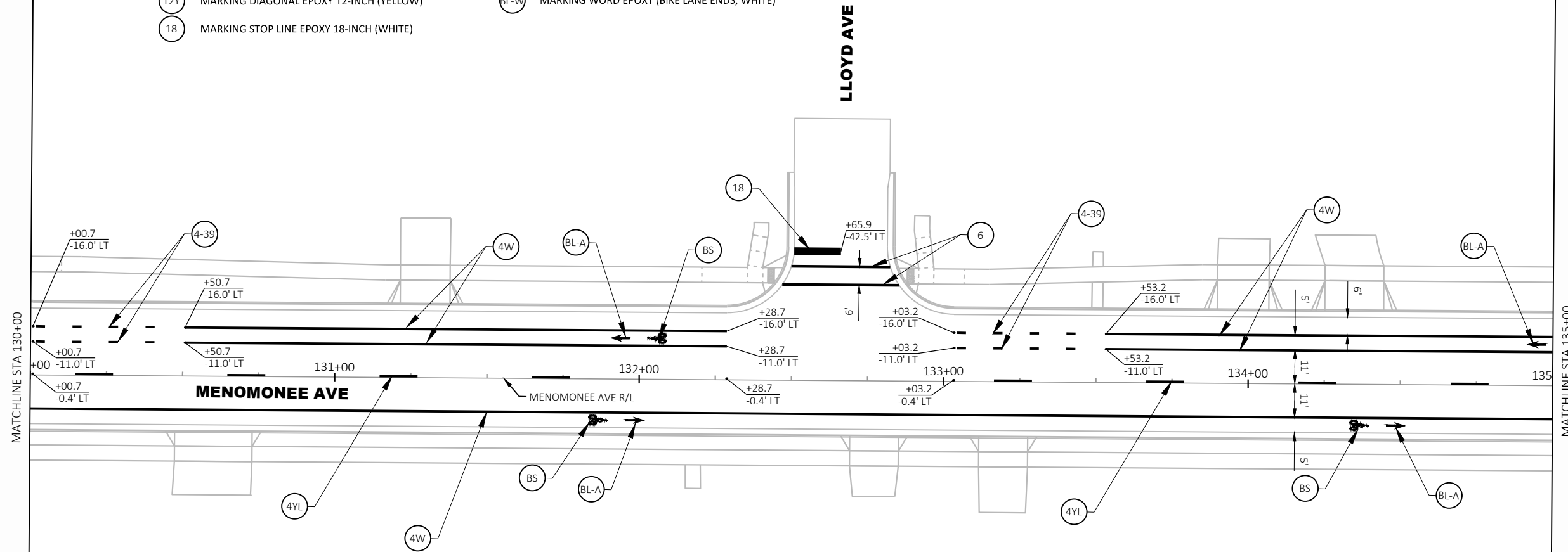
PAVEMENT MARKING LEGEND

- | | |
|---|--|
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| (18) MARKING STOP LINE EPOXY 18-INCH (WHITE) | |



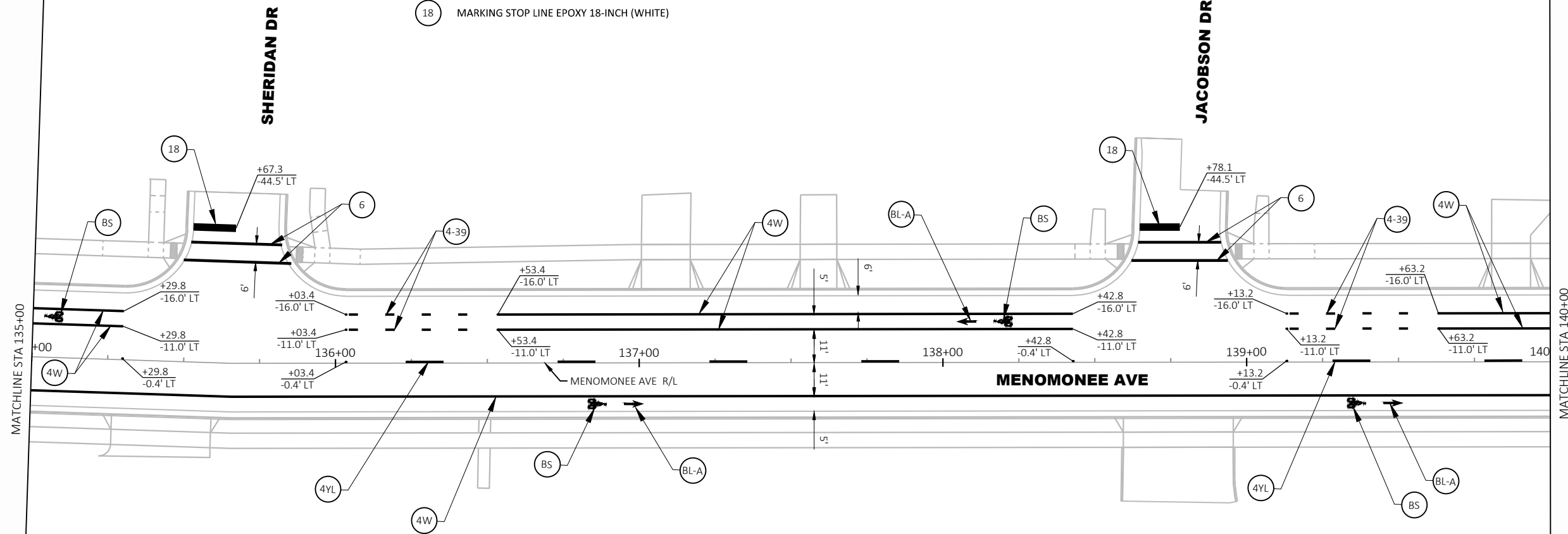
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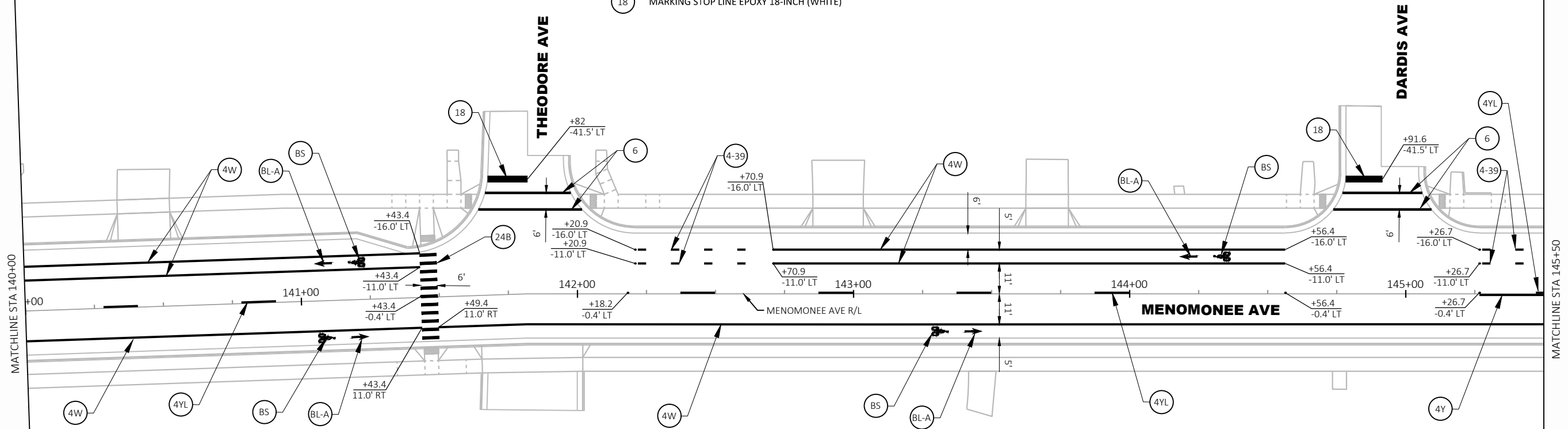
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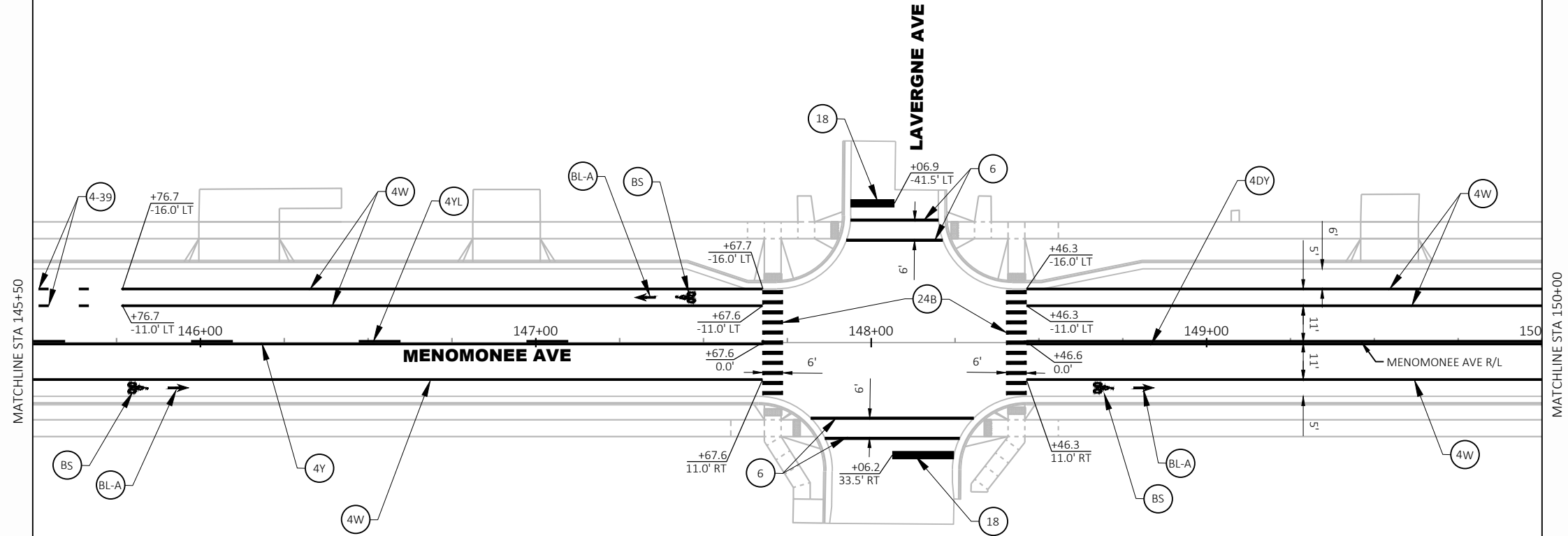
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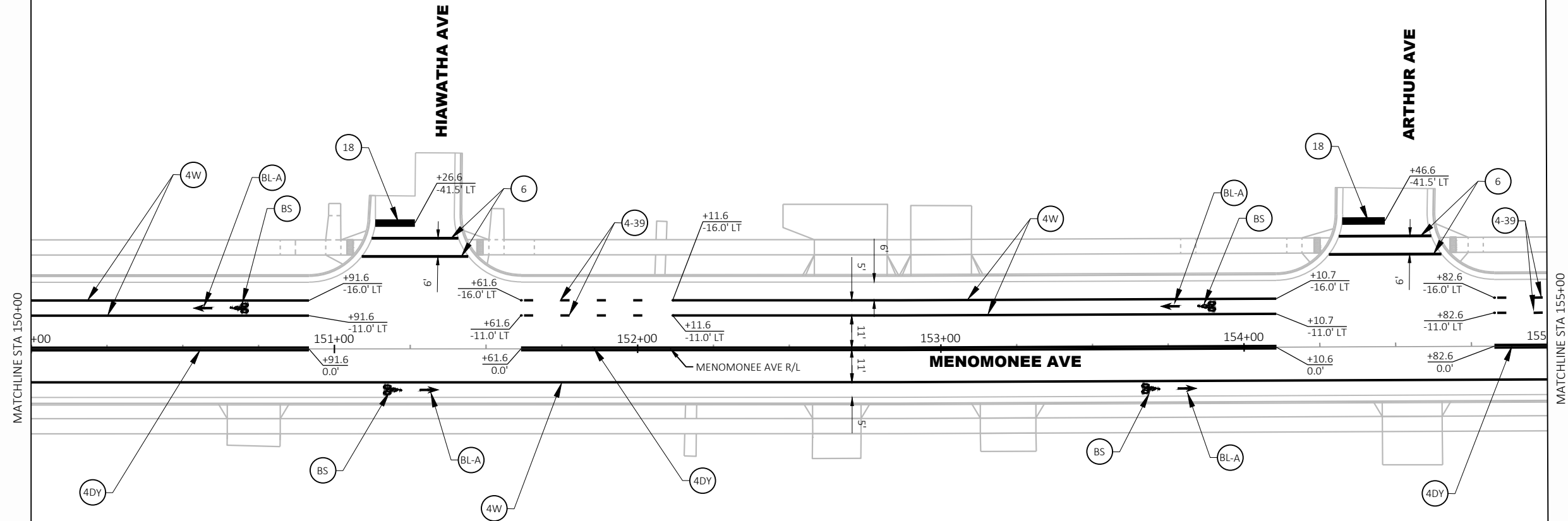
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- 18 MARKING STOP LINE EPOXY 18-INCH (WHITE)
- 6 MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)
- 24B MARKING CROSSWALK EPOXY BLOCK STYLE 24-INCH (WHITE)
- A2 MARKING ARROW EPOXY (TYPE 2, WHITE)
- A3 MARKING ARROW EPOXY (TYPE 3, WHITE)
- BS MARKING SYMBOL EPOXY (BIKE LANE, WHITE)
- BL-A MARKING SYMBOL EPOXY (BIKE LANE ARROW, WHITE)
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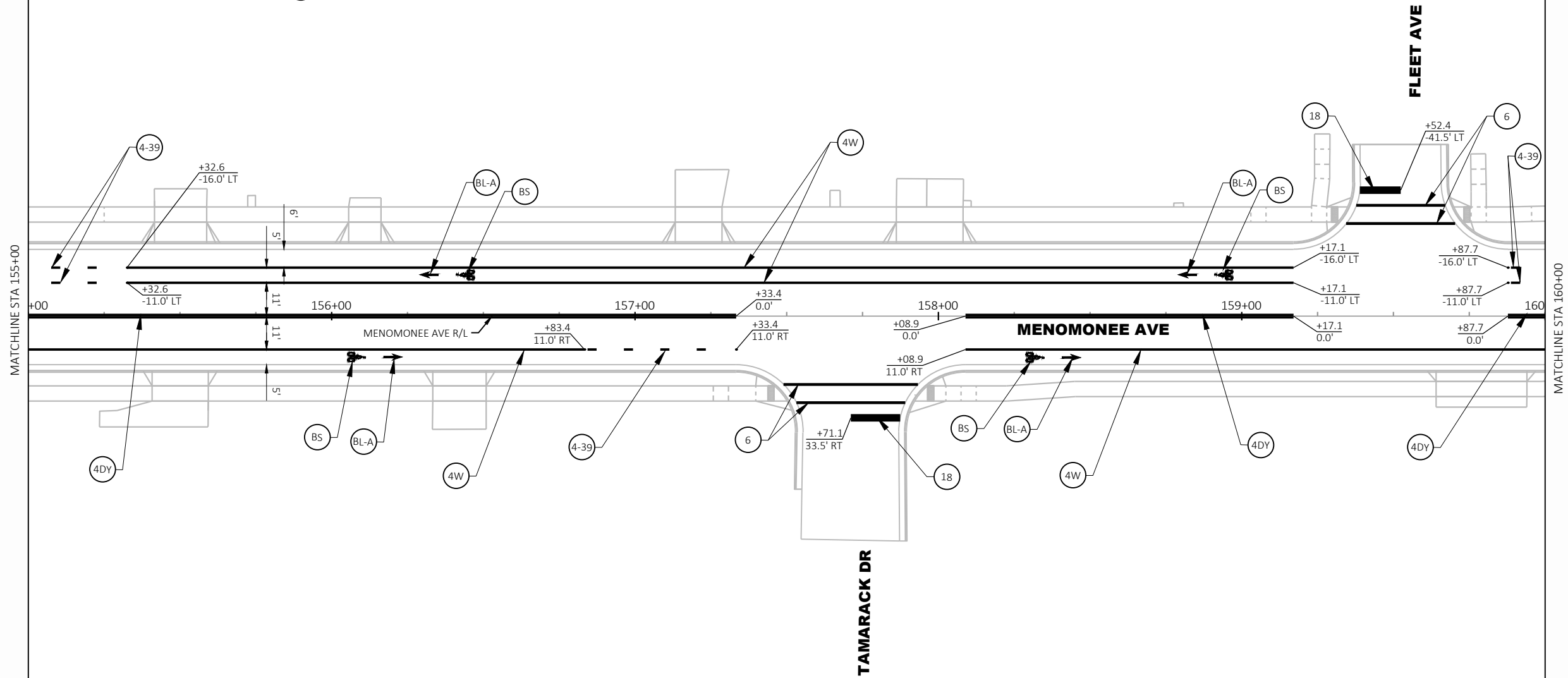


PAVEMENT MARKING LEGEND

- | | |
|---|--|
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| (4DY) MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW) | (24B) MARKING CROSSWALK EPOXY BLOCK STYLE 24-INCH (WHITE) |
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| (4W) MARKING LINE EPOXY 4-INCH (WHITE) | (A3) MARKING ARROW EPOXY (TYPE 3, WHITE) |
| (4-39) MARKING LINE EPOXY 4-INCH (3' LINE, 9' GAP, WHITE) | (BS) MARKING SYMBOL EPOXY (BIKE LANE, WHITE) |
| (8W) MARKING LINE EPOXY 8-INCH (WHITE) | (BL-A) MARKING SYMBOL EPOXY (BIKE LANE ARROW, WHITE) |
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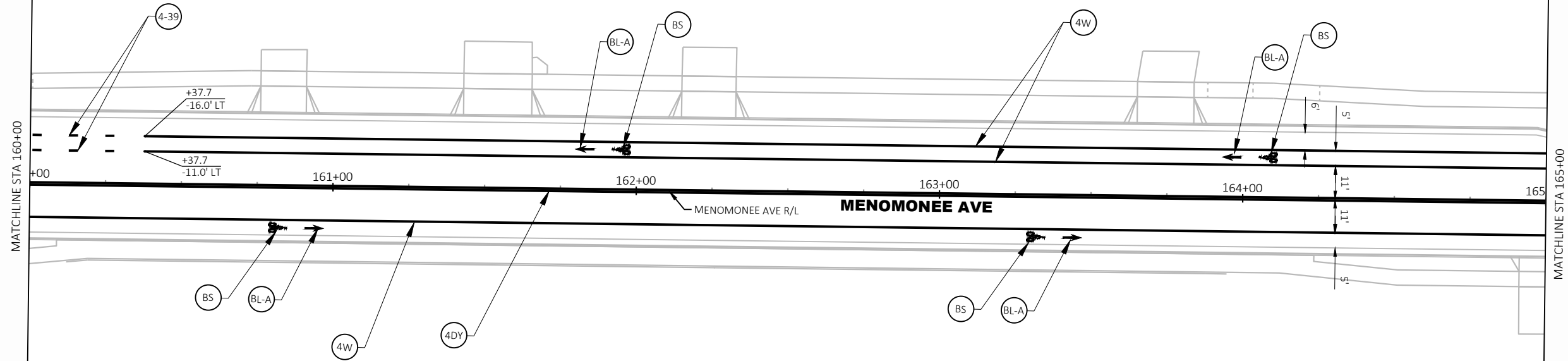
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- (4-39) MARKING LINE EPOXY 4-INCH (3' LINE, 9' GAP, WHITE)
- (8W) MARKING LINE EPOXY 8-INCH (WHITE)
- (12Y) MARKING DIAGONAL EPOXY 12-INCH (YELLOW)
- (18) MARKING STOP LINE EPOXY 18-INCH (WHITE)
- (6) MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)
- (24B) MARKING CROSSWALK EPOXY BLOCK STYLE 24-INCH (WHITE)
- (A2) MARKING ARROW EPOXY (TYPE 2, WHITE)
- (A3) MARKING ARROW EPOXY (TYPE 3, WHITE)
- (BS) MARKING SYMBOL EPOXY (BIKE LANE, WHITE)
- (BL-A) MARKING SYMBOL EPOXY (BIKE LANE ARROW, WHITE)
- (BL-W) MARKING WORD EPOXY (BIKE LANE ENDS, WHITE)



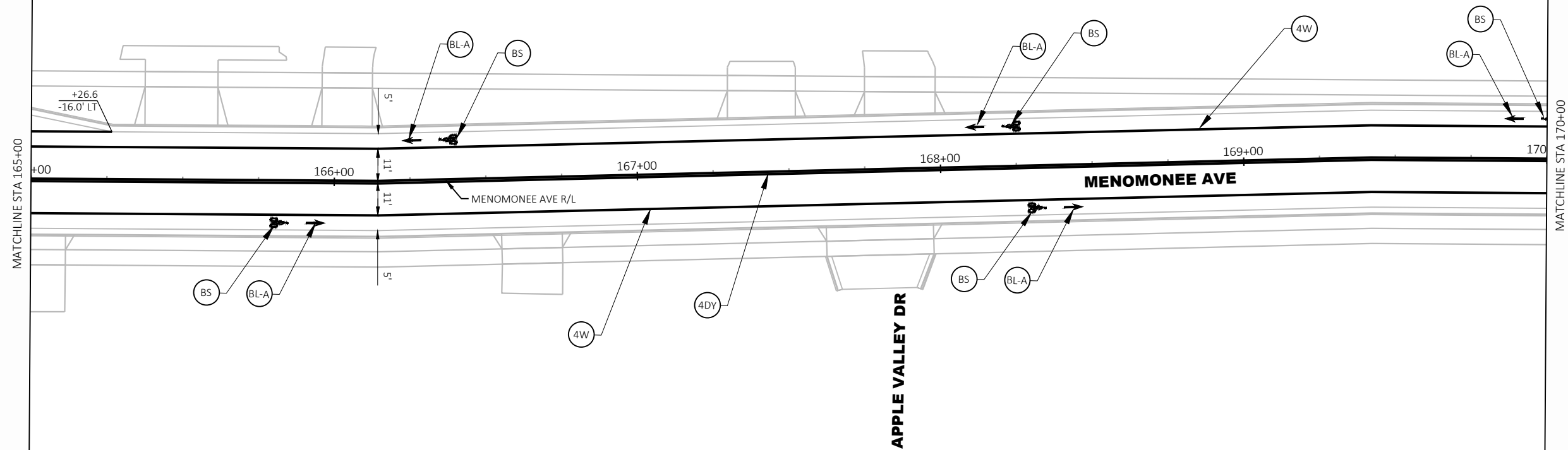
PAVEMENT MARKING LEGEND

- (4Y) MARKING LINE EPOXY 4-INCH (YELLOW)
- (4DY) MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW)
- (4YL) MARKING LINE EPOXY 4-INCH (12.5' LINE, 37.5' GAP, YELLOW)
- (4W) MARKING LINE EPOXY 4-INCH (WHITE)
- (4-39) MARKING LINE EPOXY 4-INCH (3' LINE, 9' GAP, WHITE)
- (8W) MARKING LINE EPOXY 8-INCH (WHITE)
- (12Y) MARKING DIAGONAL EPOXY 12-INCH (YELLOW)
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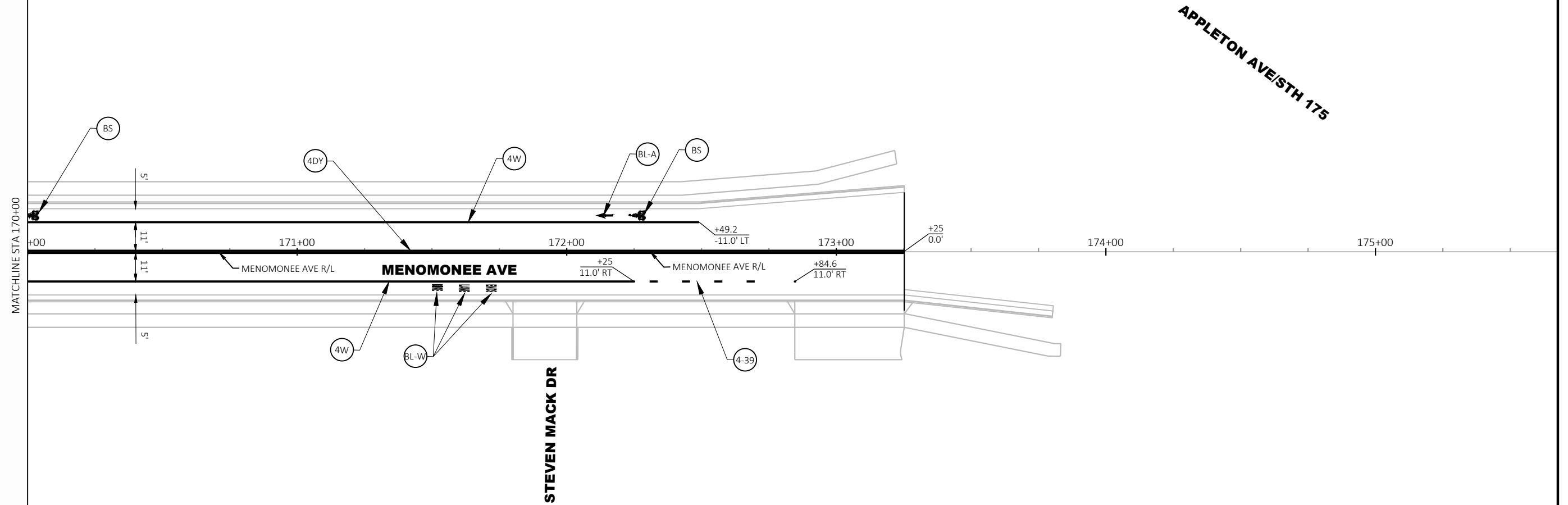
PAVEMENT MARKING LEGEND



- (4Y) MARKING LINE EPOXY 4-INCH (YELLOW)
- (4DY) MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW)
- (4YL) MARKING LINE EPOXY 4-INCH (12.5' LINE, 37.5' GAP, YELLOW)
- (4W) MARKING LINE EPOXY 4-INCH (WHITE)
- (4-39) MARKING LINE EPOXY 4-INCH (3' LINE, 9' GAP, WHITE)
- (8W) MARKING LINE EPOXY 8-INCH (WHITE)
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- (BL-W) MARKING WORD EPOXY (BIKE LANE ENDS, WHITE)

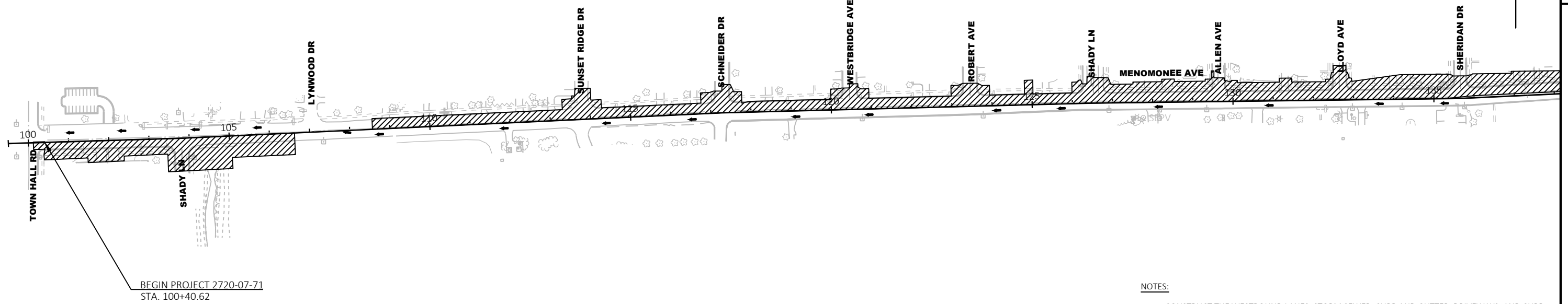


PAVEMENT MARKING LEGEND

- (4Y) MARKING LINE EPOXY 4-INCH (YELLOW)
- (4DY) MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW)
- (4YL) MARKING LINE EPOXY 4-INCH (12.5' LINE, 37.5' GAP, YELLOW)
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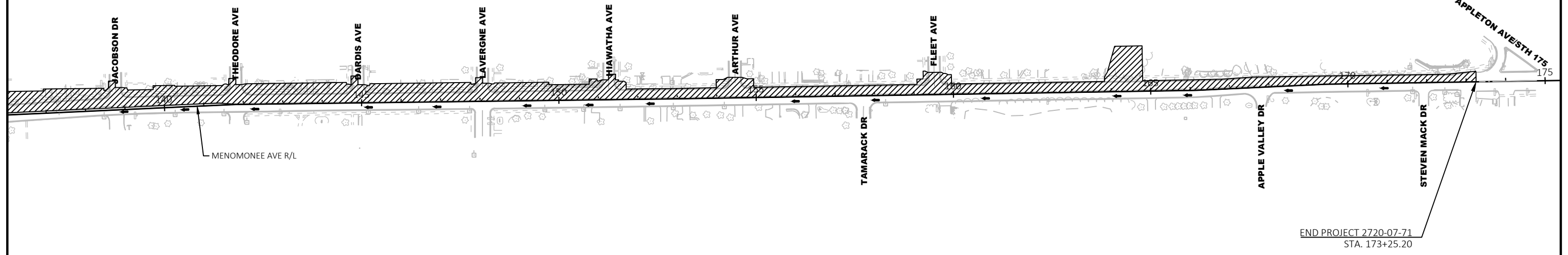




LEGEND
 WORK AREA
 DIRECTION OF TRAFFIC

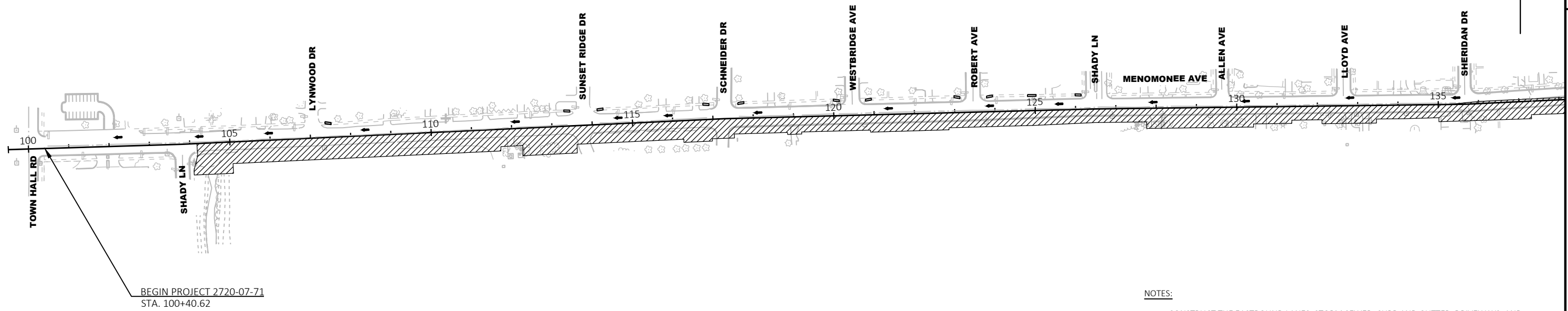


NOTES:

- CONSTRUCT THE WESTBOUND LANES, STORM SEWER, CURB AND GUTTER, DRIVEWAYS, AND CURB RAMPS. TRAFFIC WILL UTILIZE THE EASTBOUND LANES TO TRAVEL WESTBOUND.
- DETOUR EASTBOUND TRAFFIC VIA TOWN HALL ROAD, MAIN STREET, AND APPLETON AVE/STH 175.
- MAINTAIN PEDESTRIAN ACCESS AT ALL TIMES ALONG THE NORTH SIDE OF MENOMONEE AVENUE ON THE EXISTING SIDEWALK AND THROUGH THE WORK ZONE UTILIZING TEMP CURB RAMPS AND PEDESTRIAN BARRICADE AND PEDESTRIAN DETOURS.

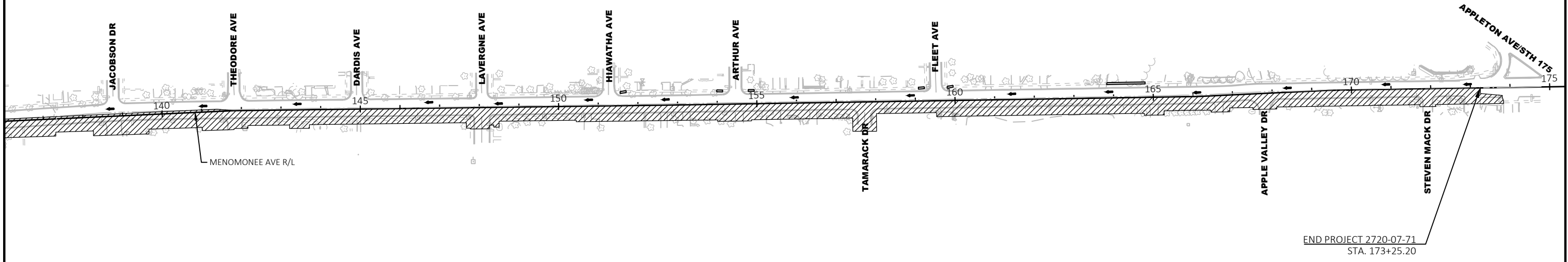


LEGEND
 WORK AREA
 DIRECTION OF TRAFFIC





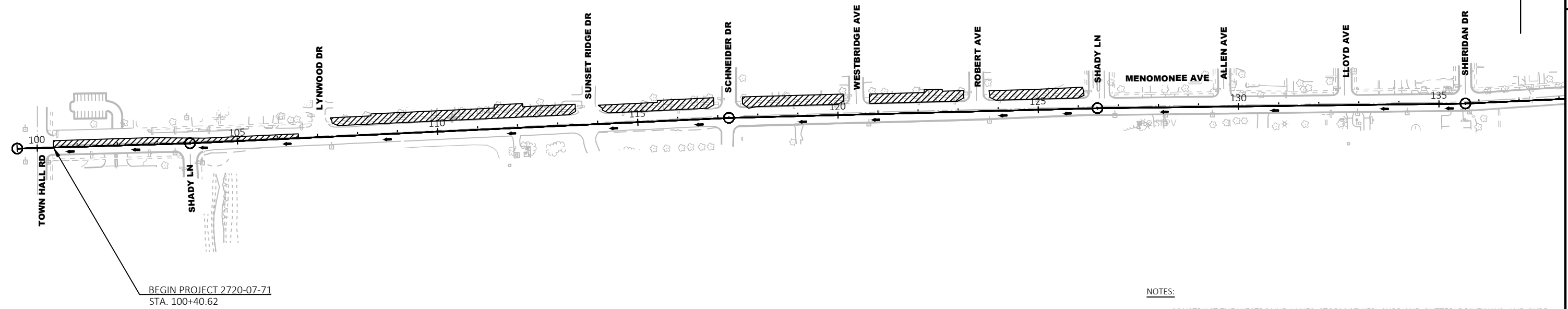
NOTES:

- CONSTRUCT THE EASTBOUND LANES, STORM SEWER, CURB AND GUTTER, DRIVEWAYS, AND SIDEWALKS. TRAFFIC WILL UTILIZE THE WESTBOUND LANES.
- DETOUR EASTBOUND TRAFFIC VIA TOWN HALL ROAD, MAIN STREET, AND APPLETON AVE/STH 175.
- MAINTAIN PEDESTRIAN ACCESS AT ALL TIMES ALONG THE NORTH SIDE OF MENOMONEE AVENUE WHERE THERE IS EXISTING SIDEWALK AND THROUGH THE WORK ZONE UTILIZING TEMP CURB RAMPS AND PEDESTRIAN BARRICADE.



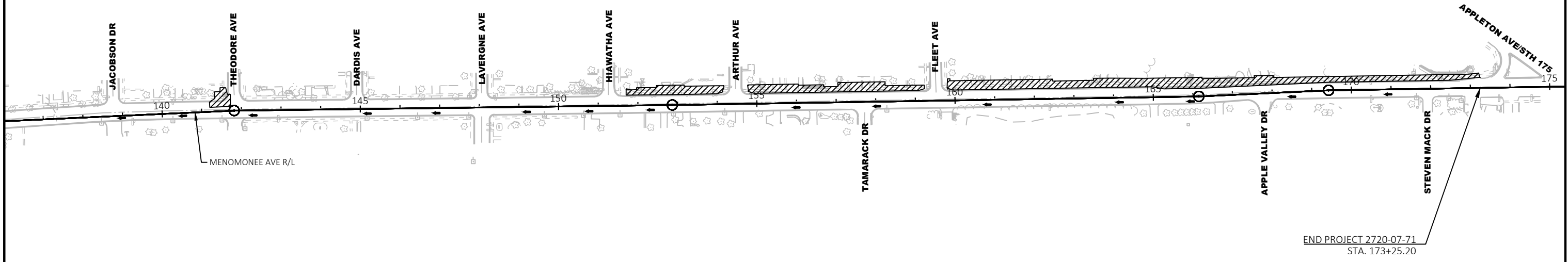
PROJECT NO: 2720-07-71	HWY: MENOMONEE AVE	COUNTY: WAUKESHA	TRAFFIC CONTROL OVERVIEW - STAGE 2	SHEET	E
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LEGEND
 WORK AREA
 DIRECTION OF TRAFFIC

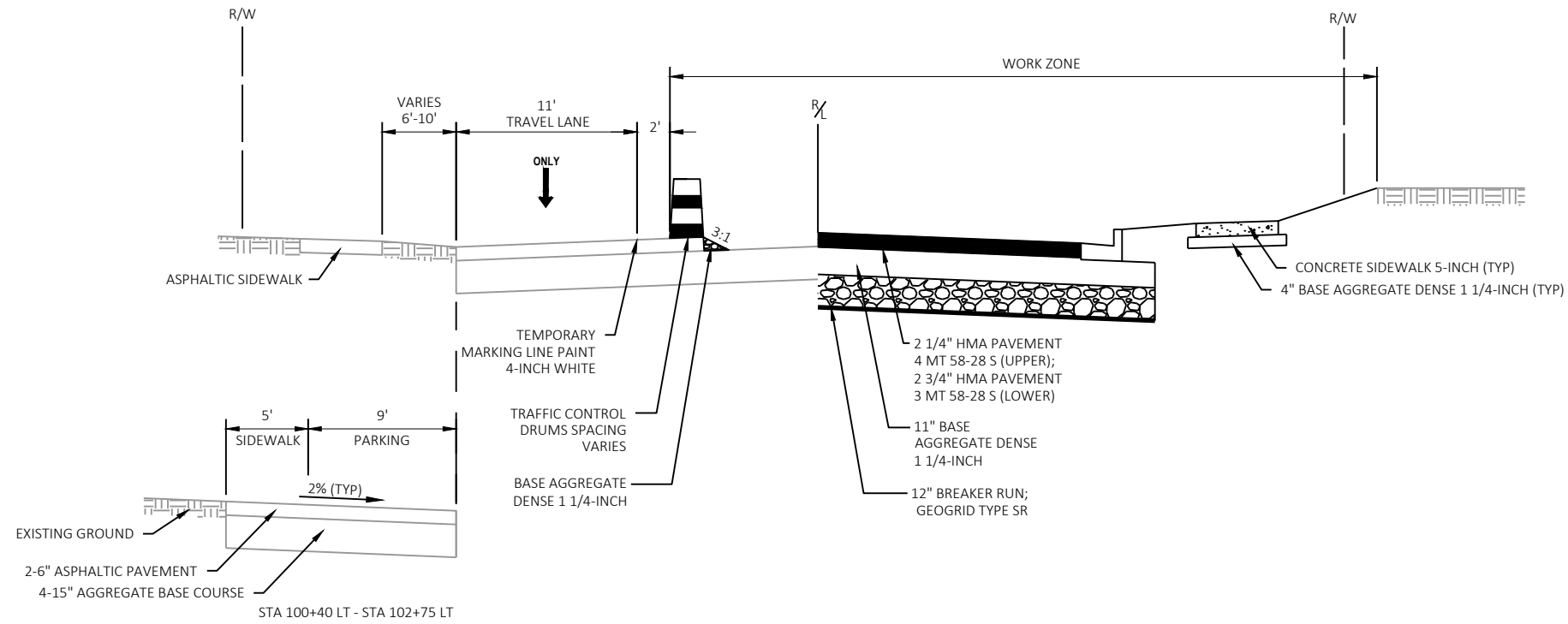


NOTES:

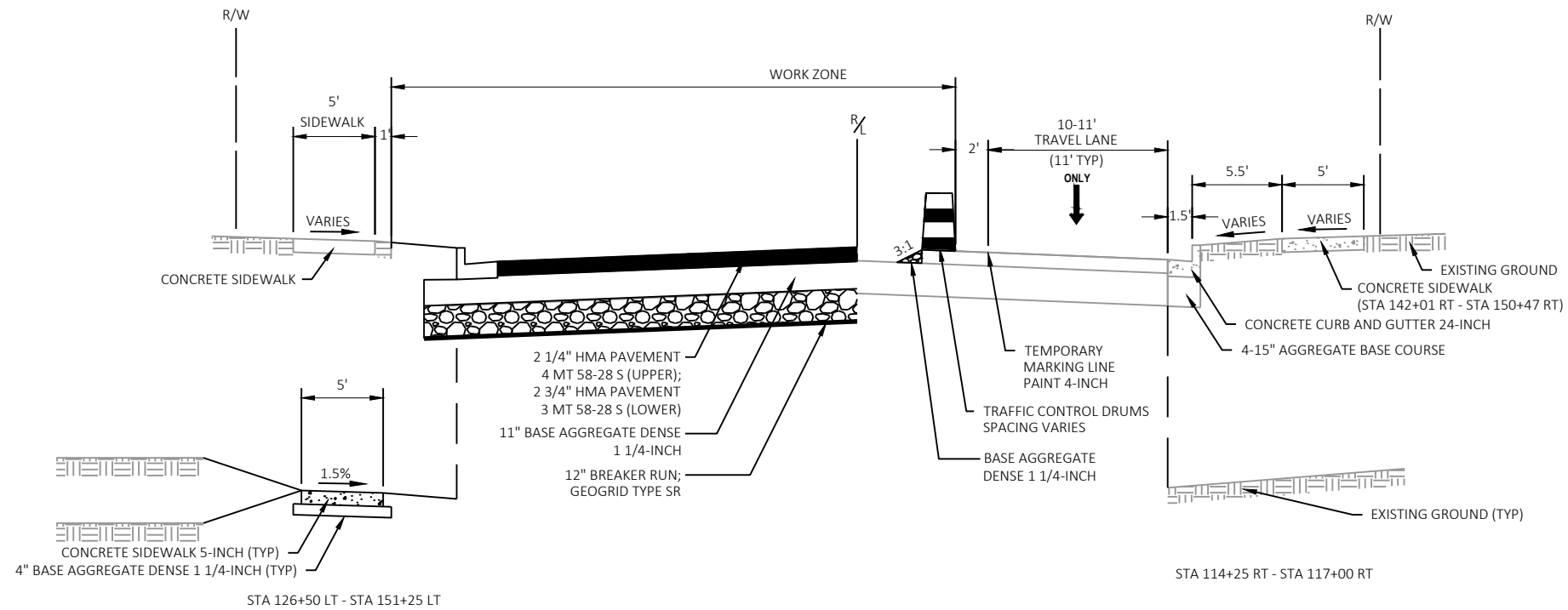
- CONSTRUCT THE WESTBOUND LANES, STORM SEWER, CURB AND GUTTER, DRIVEWAYS, AND CURB RAMPS. TRAFFIC WILL UTILIZE THE EASTBOUND LANES TO TRAVEL WESTBOUND.
- DETOUR EASTBOUND TRAFFIC VIA TOWN HALL ROAD, MAIN STREET, AND APPLETON AVE/STH 175.
- MAINTAIN PEDESTRIAN ACCESS AT ALL TIMES ALONG THE NORTH SIDE OF MENOMONEE AVENUE AND THROUGH THE WORK ZONE UTILIZING TEMP CURB RAMPS AND PEDESTRIAN BARRICADE BY ROUTING PEDESTRIANS TO THE COMPLETED SIDEWALK ON THE SOUTH SIDE OF MENOMONEE.



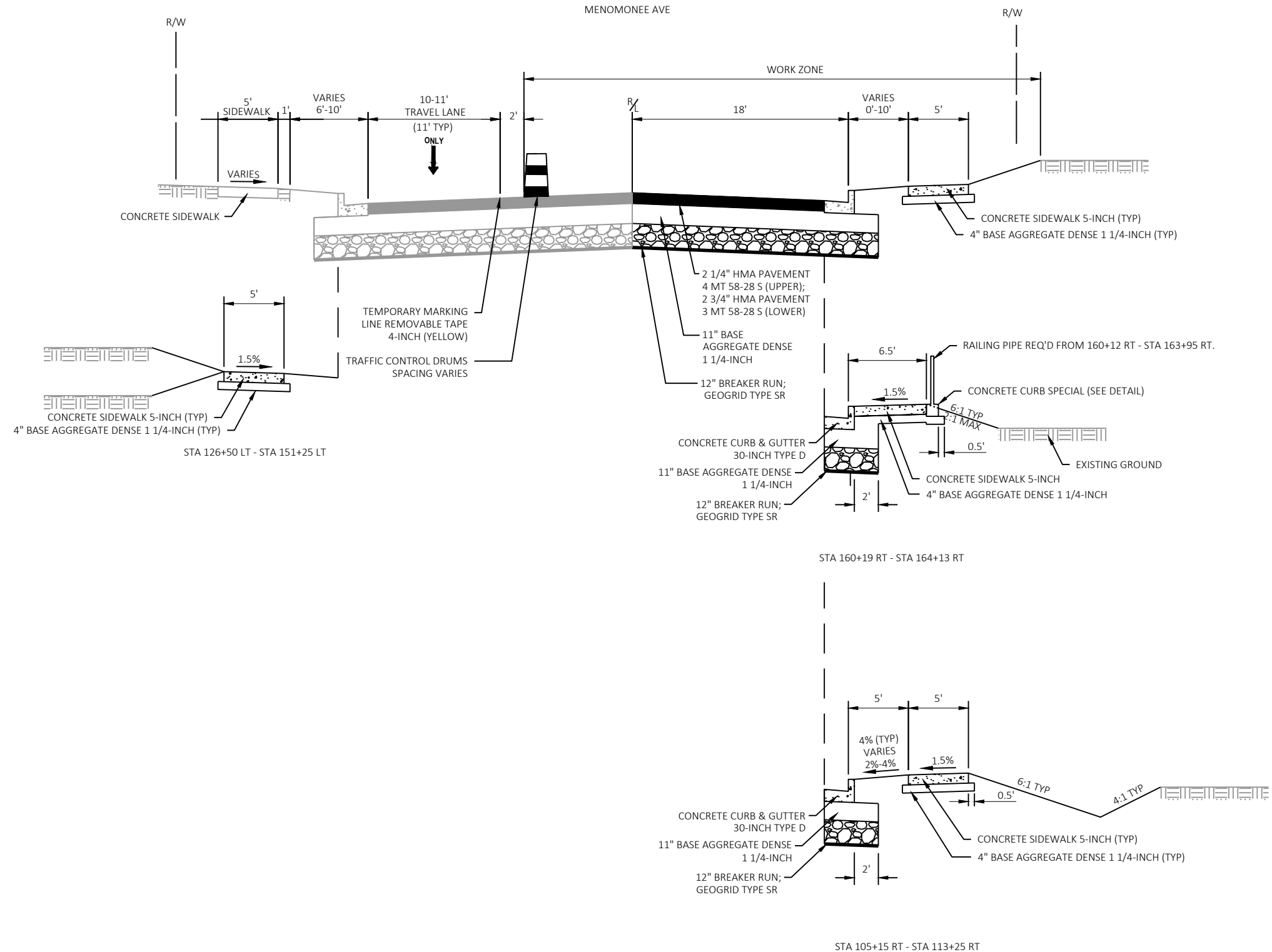
PROJECT NO: 2720-07-71	HWY: MENOMONEE AVE	COUNTY: WAUKESHA	TRAFFIC CONTROL OVERVIEW - STAGE 3	SHEET	E
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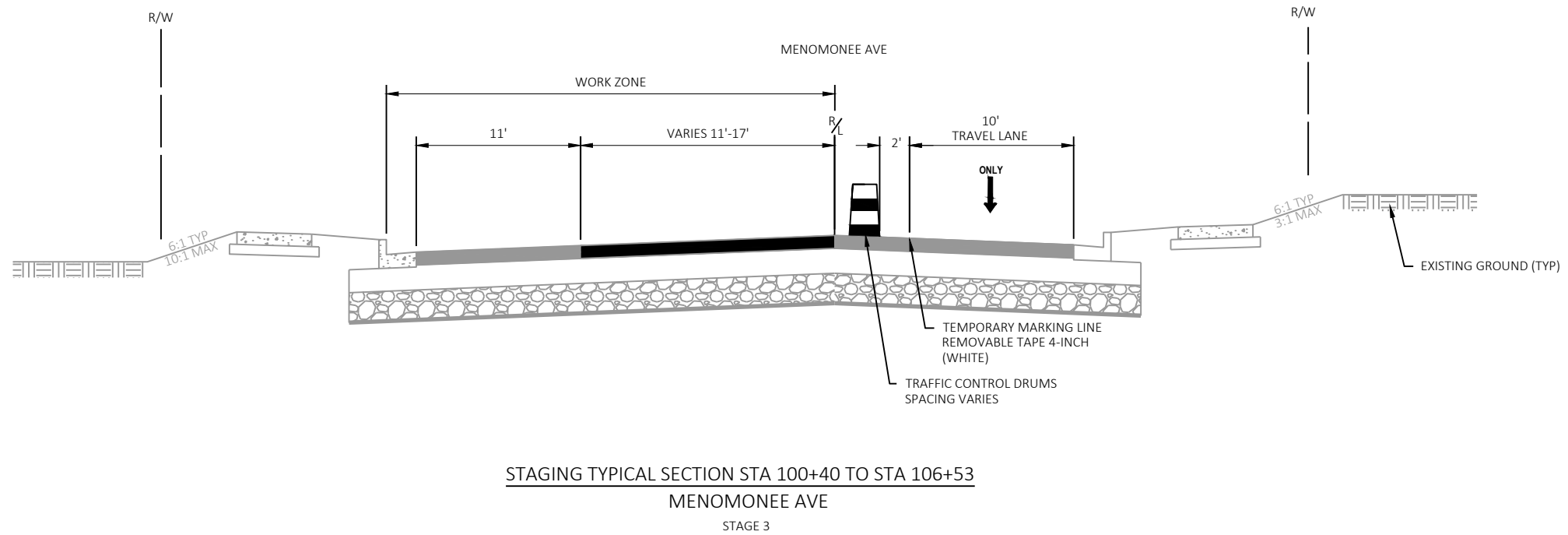
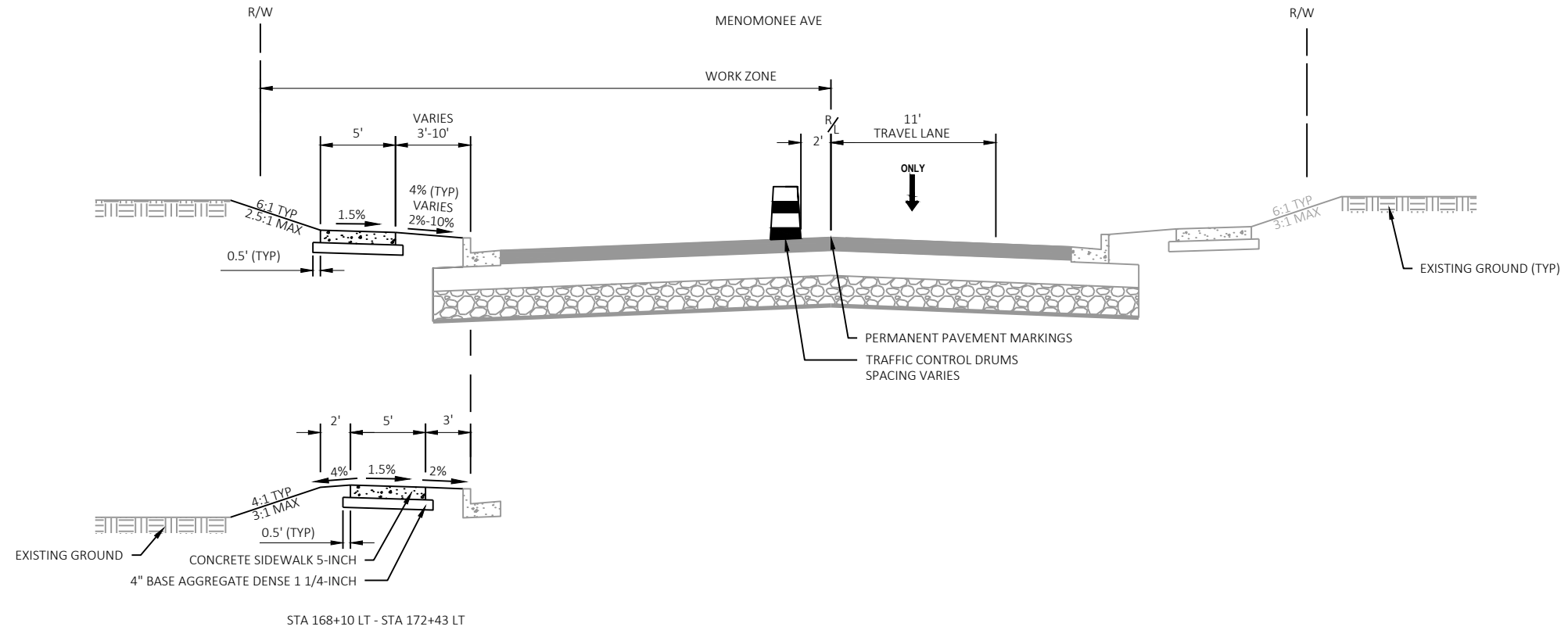
STAGING TYPICAL SECTION STA 100+40 TO STA 103+30
 MENOMONEE AVE
 STAGE 1A



STAGING TYPICAL SECTION STA 108+55 TO STA 173+25
 MENOMONEE AVE
 STAGE 1B



STAGING TYPICAL SECTION STA 103+30 TO STA 173+25
MENOMONEE AVE
STAGE 2



GENERAL NOTES:
 ALL SIGNS SHALL BE 48" X 48" UNLESS NOTED OTHERWISE.

ACTUAL PORTABLE CHANGEABLE MESSAGE SIGN LOCATIONS AND MESSAGES SHALL BE APPROVED BY THE ENGINEER.

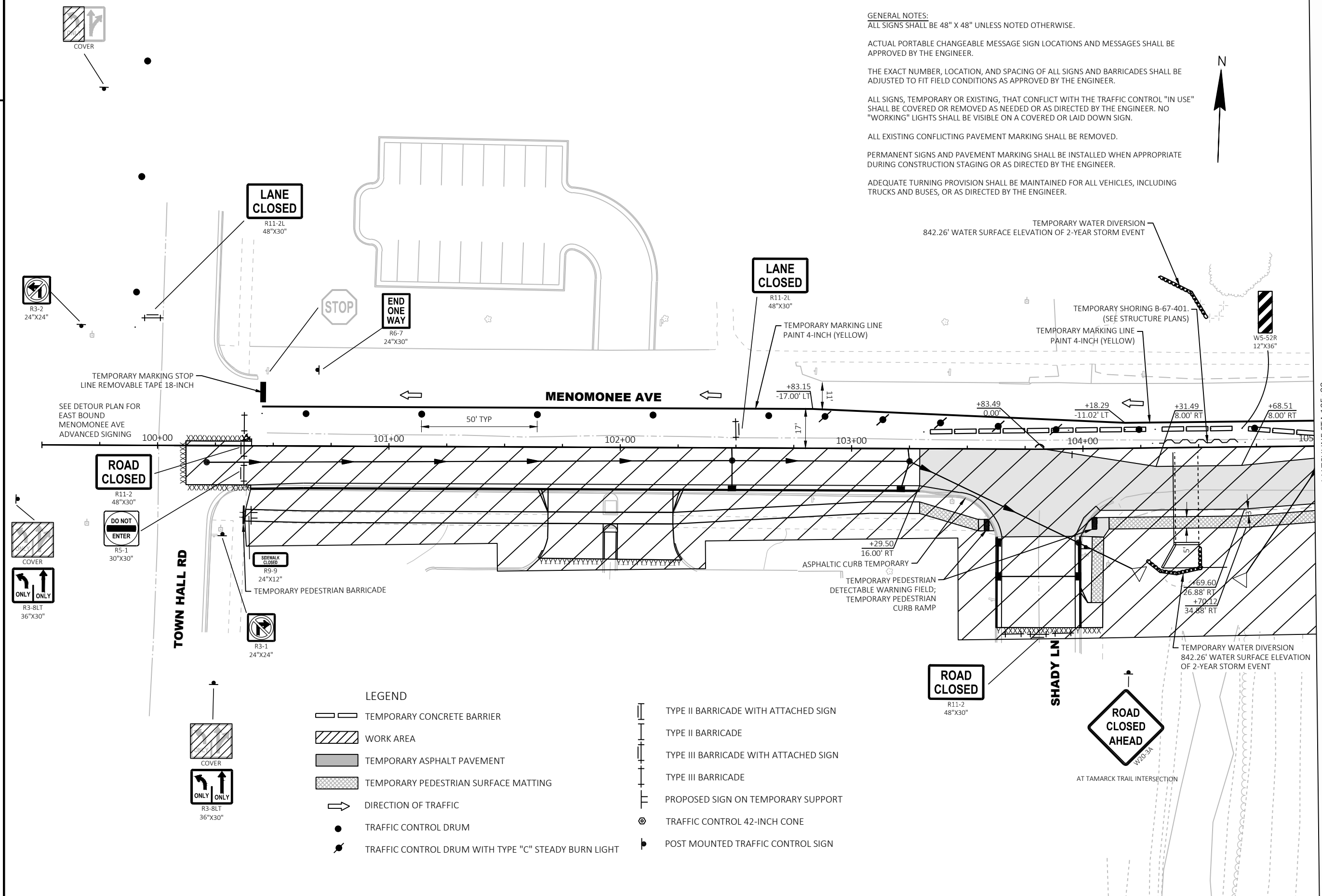
THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ALL SIGNS, TEMPORARY OR EXISTING, THAT CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE COVERED OR REMOVED AS NEEDED OR AS DIRECTED BY THE ENGINEER. NO "WORKING" LIGHTS SHALL BE VISIBLE ON A COVERED OR LAID DOWN SIGN.

ALL EXISTING CONFLICTING PAVEMENT MARKING SHALL BE REMOVED.















PERMANENT SIGNS AND PAVEMENT MARKING SHALL BE INSTALLED WHEN APPROPRIATE DURING CONSTRUCTION STAGING OR AS DIRECTED BY THE ENGINEER.

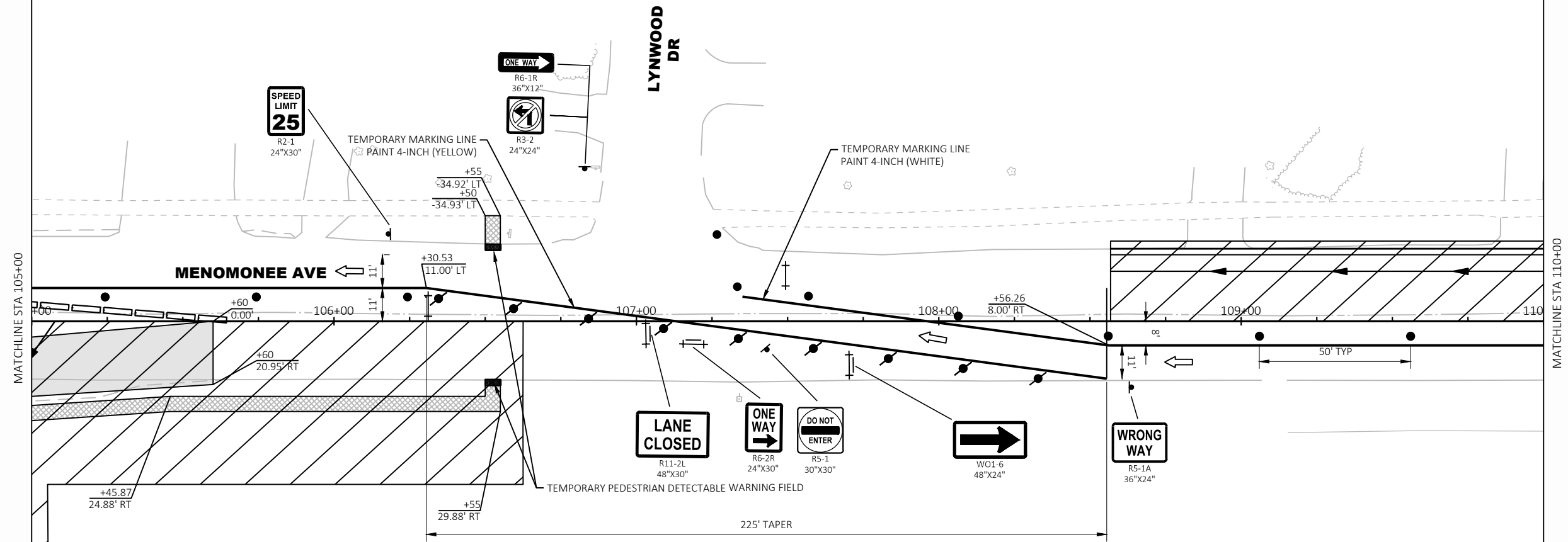
ADEQUATE TURNING PROVISION SHALL BE MAINTAINED FOR ALL VEHICLES, INCLUDING TRUCKS AND BUSES, OR AS DIRECTED BY THE ENGINEER.

















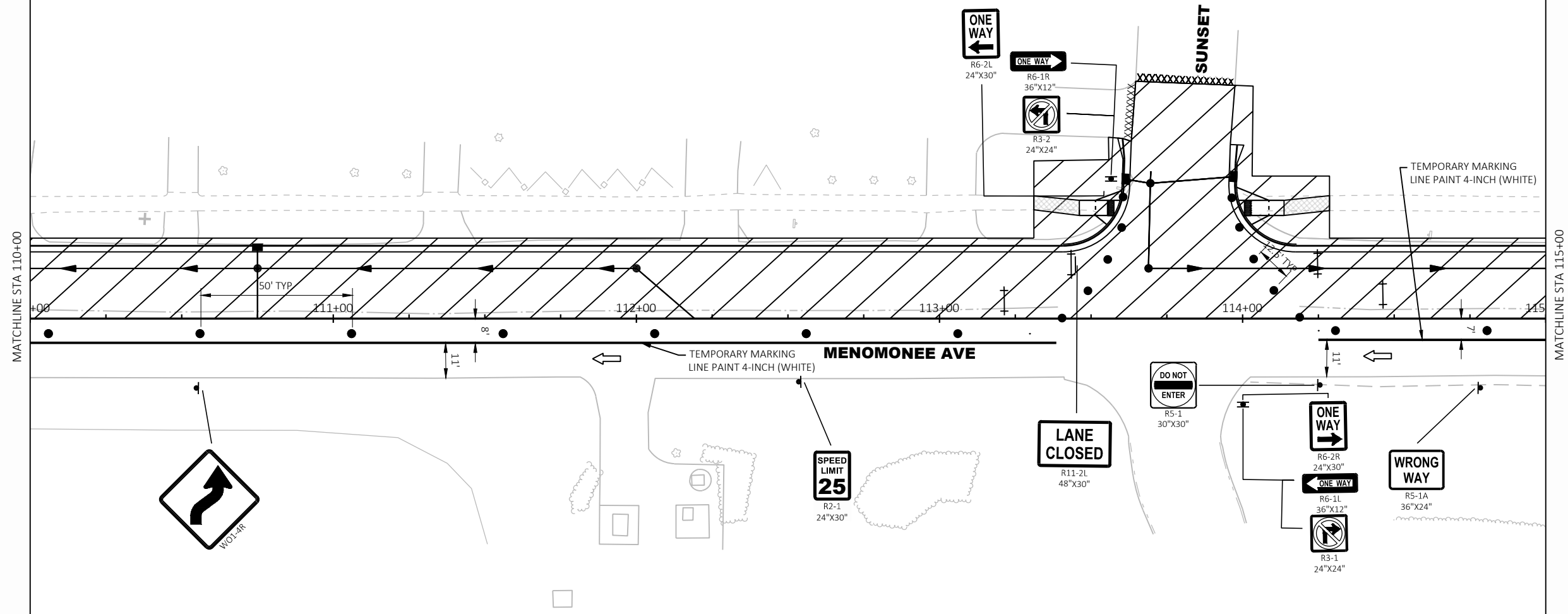
LEGEND















- TEMPORARY CONCRETE BARRIER
- WORK AREA
- TEMPORARY ASPHALT PAVEMENT
- TEMPORARY PEDESTRIAN SURFACE MATTING
- DIRECTION OF TRAFFIC
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- TYPE II BARRICADE WITH ATTACHED SIGN
- TYPE II BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE III BARRICADE
- PROPOSED SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL 42-INCH CONE
- POST MOUNTED TRAFFIC CONTROL SIGN

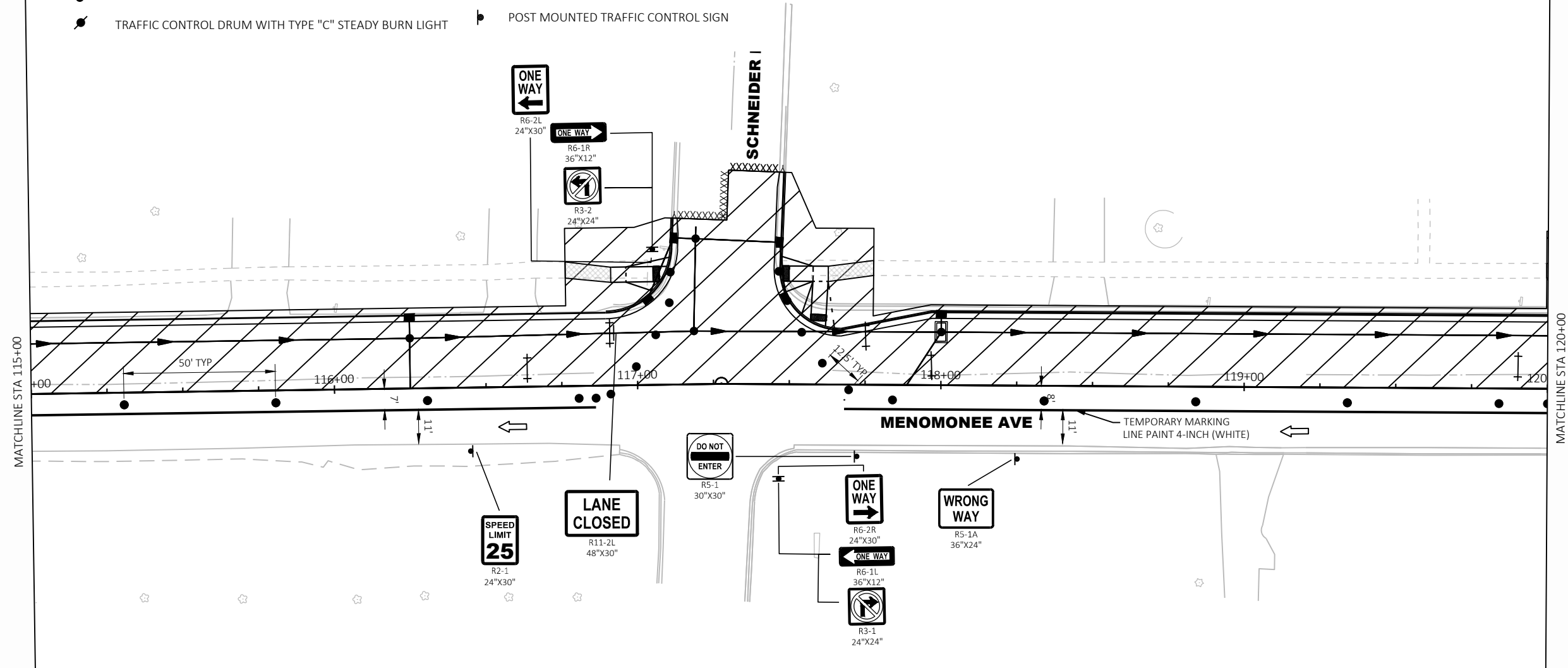
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




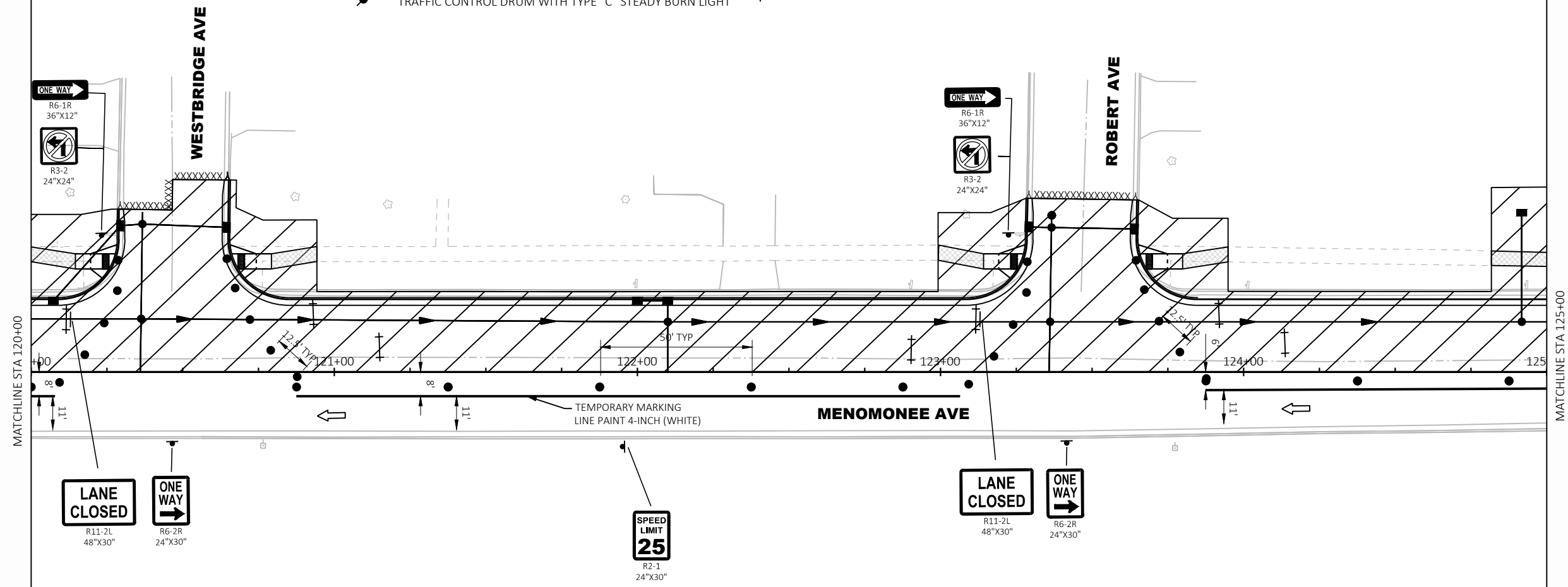
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









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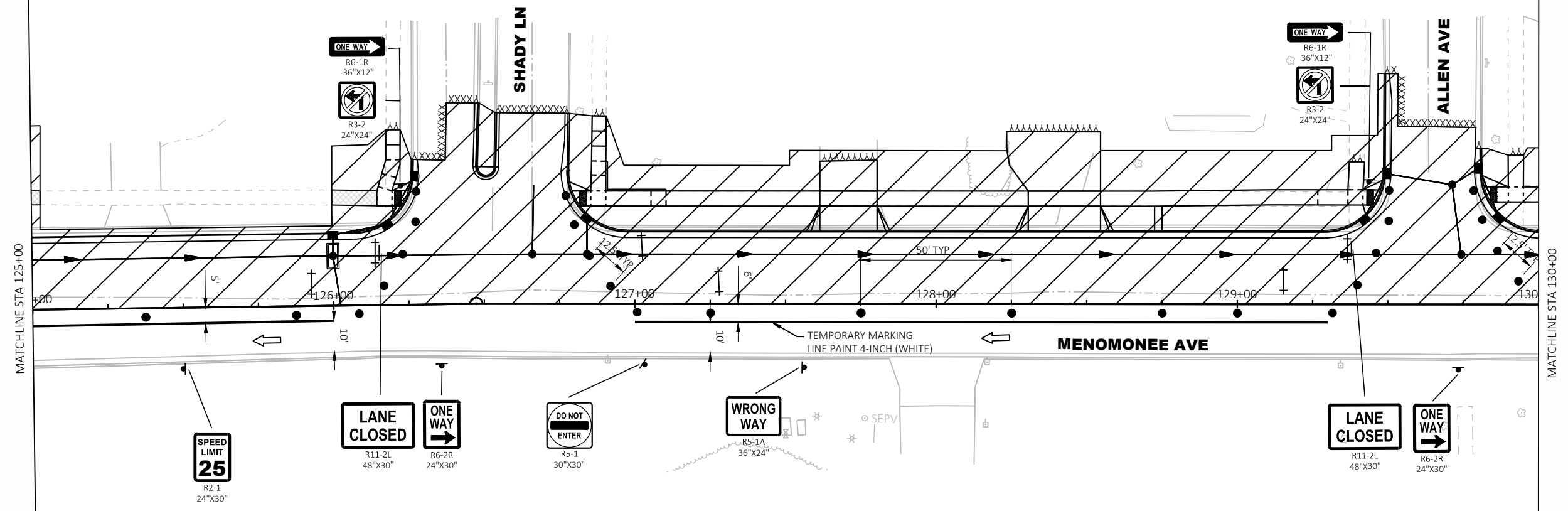
















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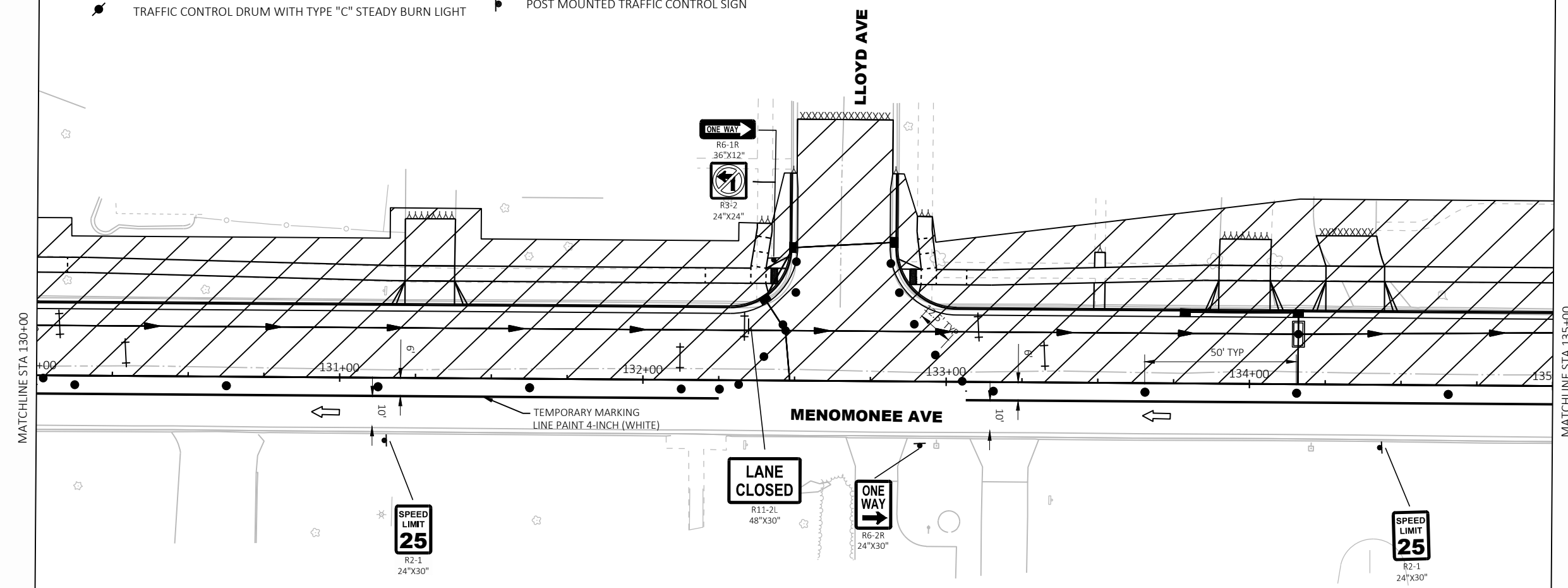


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













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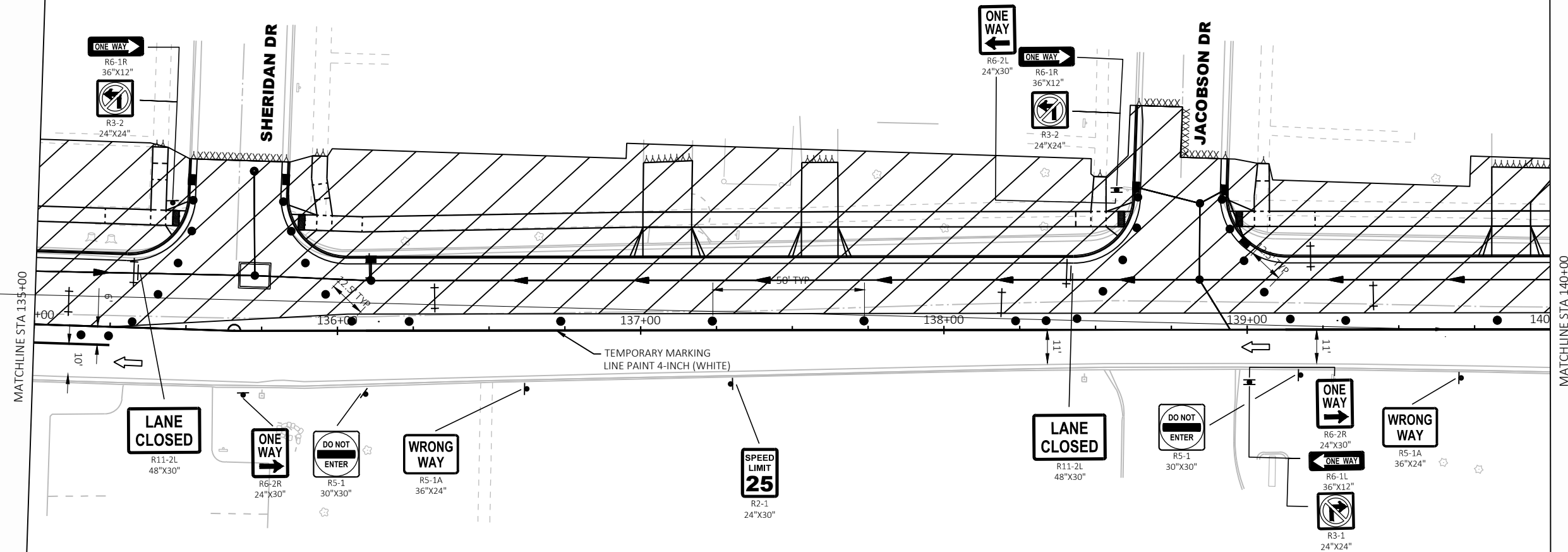


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















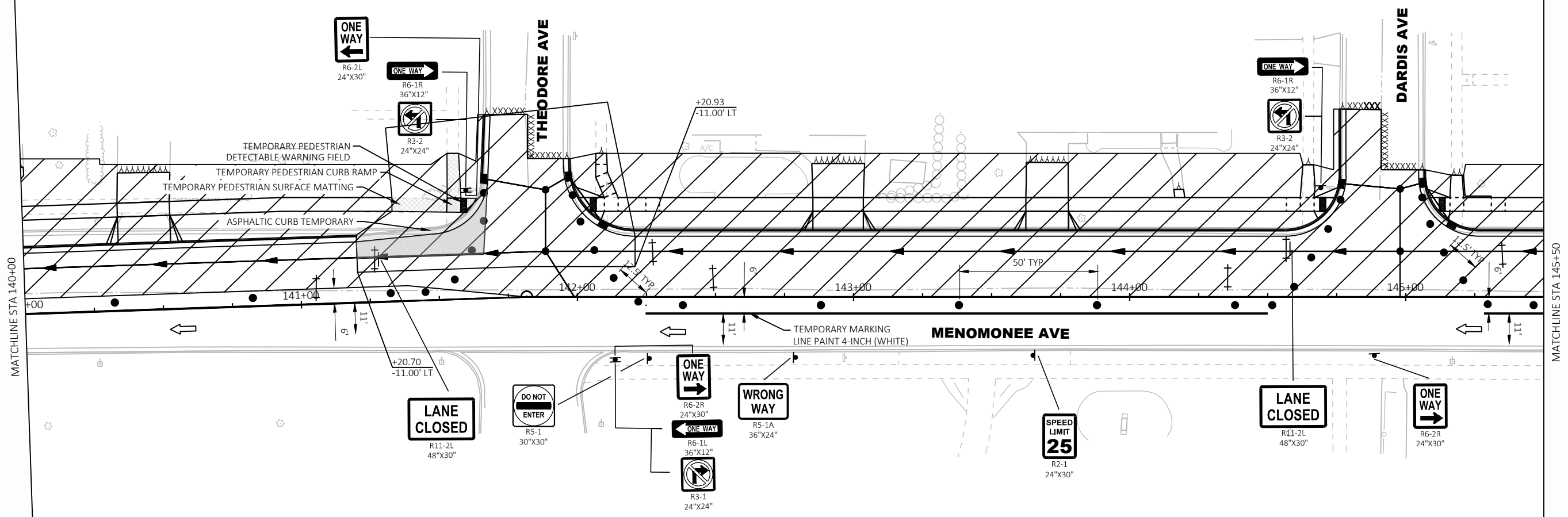
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













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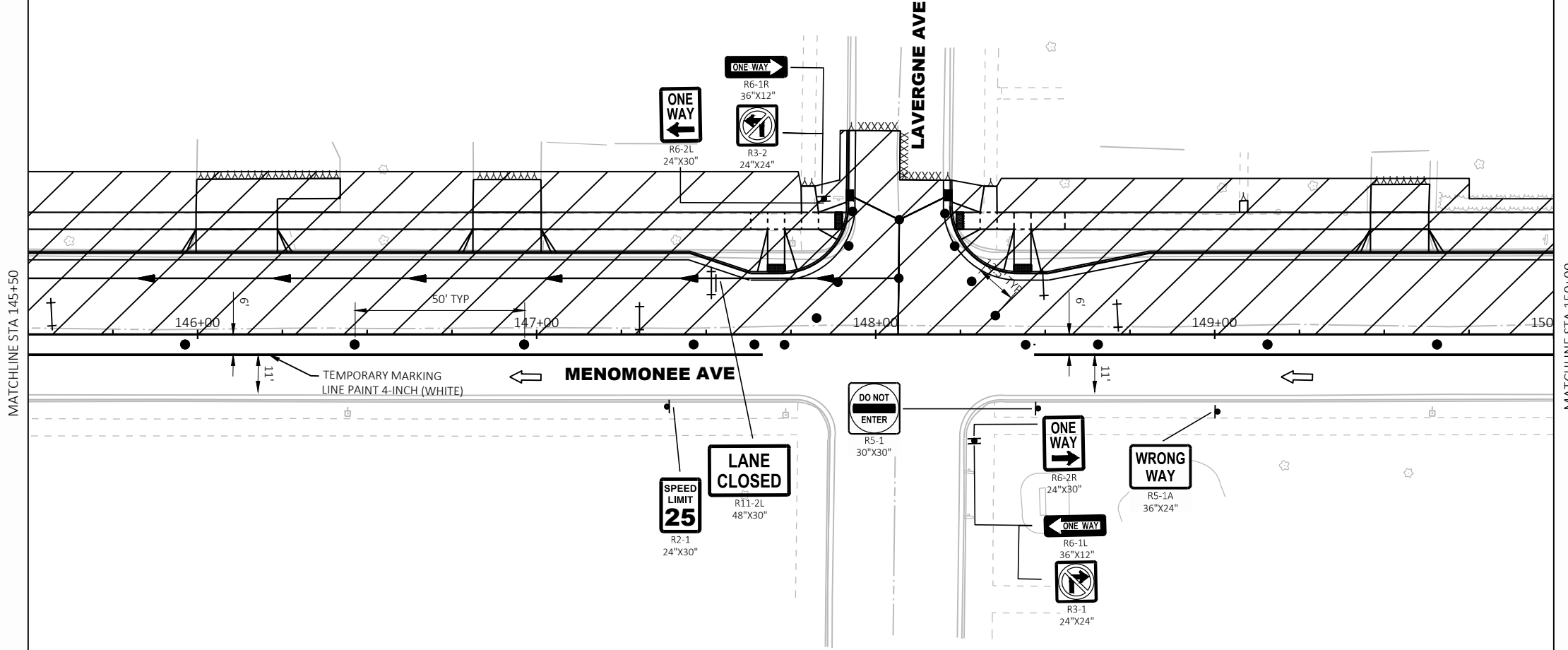


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













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-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
-  TYPE II BARRICADE WITH ATTACHED SIGN
-  TYPE II BARRICADE
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  TYPE III BARRICADE
-  PROPOSED SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL 42-INCH CONE
-  POST MOUNTED TRAFFIC CONTROL SIGN

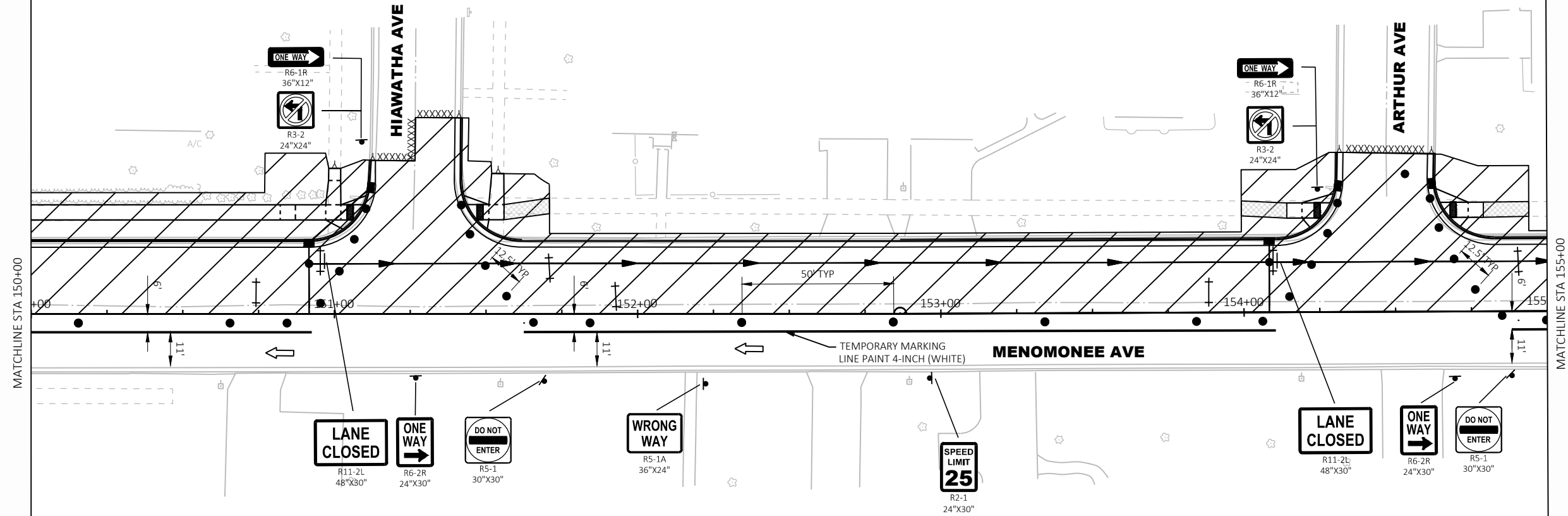
















- LEGEND**
-  TEMPORARY CONCRETE BARRIER
 -  WORK AREA
 -  TEMPORARY ASPHALT PAVEMENT
 -  TEMPORARY PEDESTRIAN SURFACE MATTING
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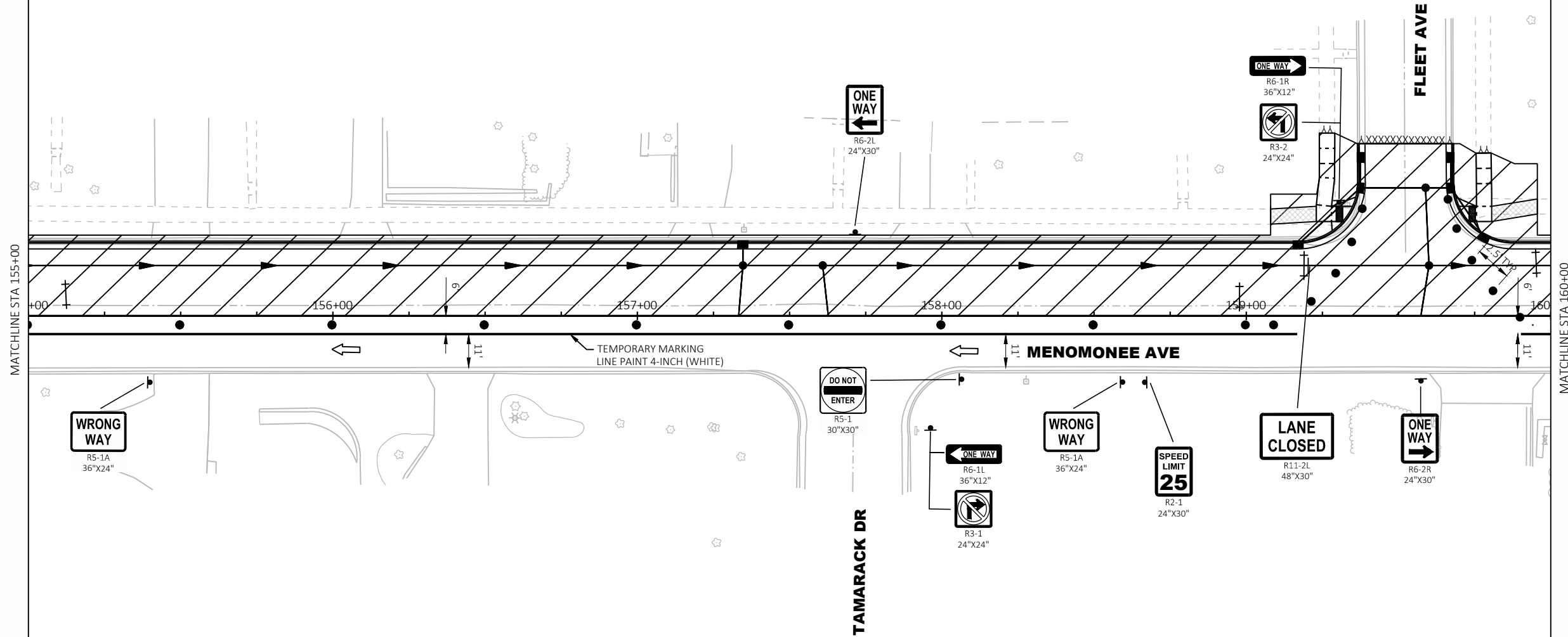


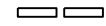













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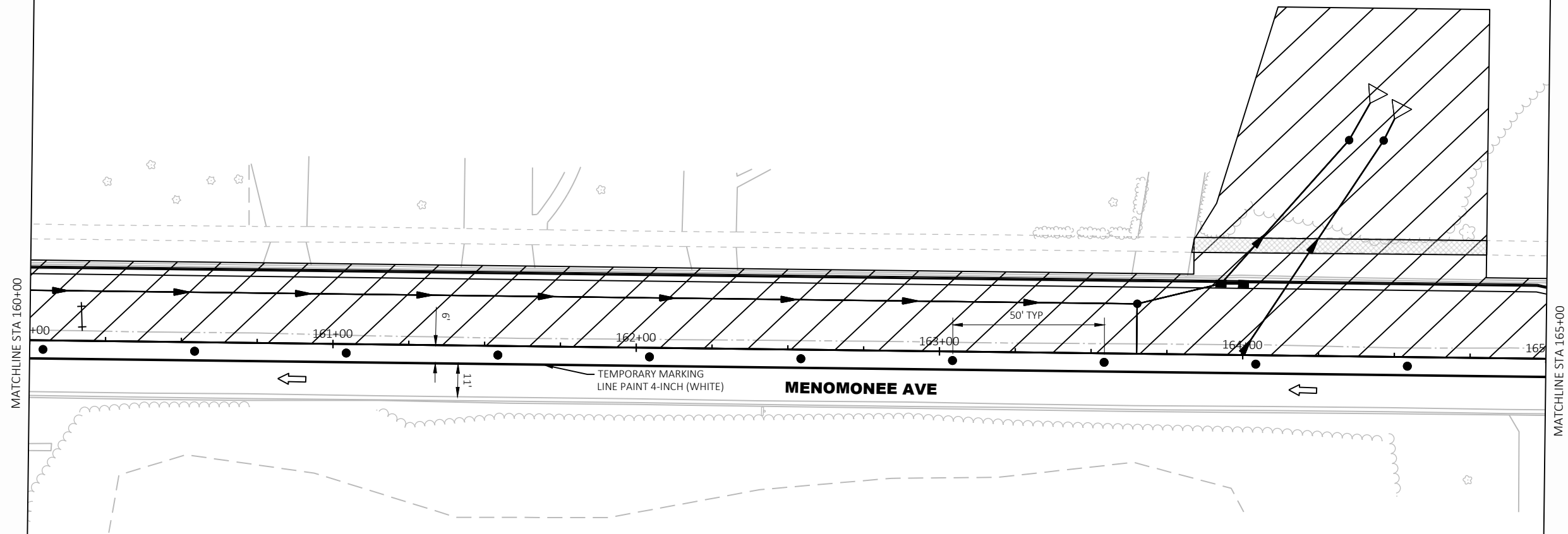
-  TEMPORARY CONCRETE BARRIER
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-  TEMPORARY PEDESTRIAN SURFACE MATTING
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















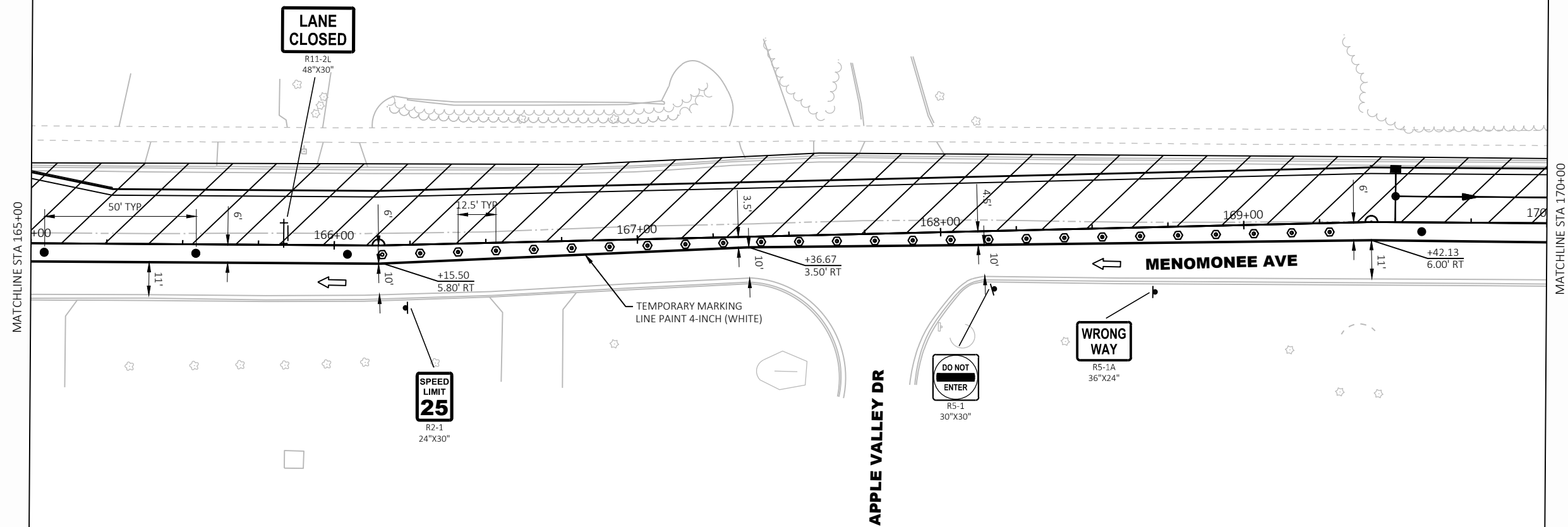
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








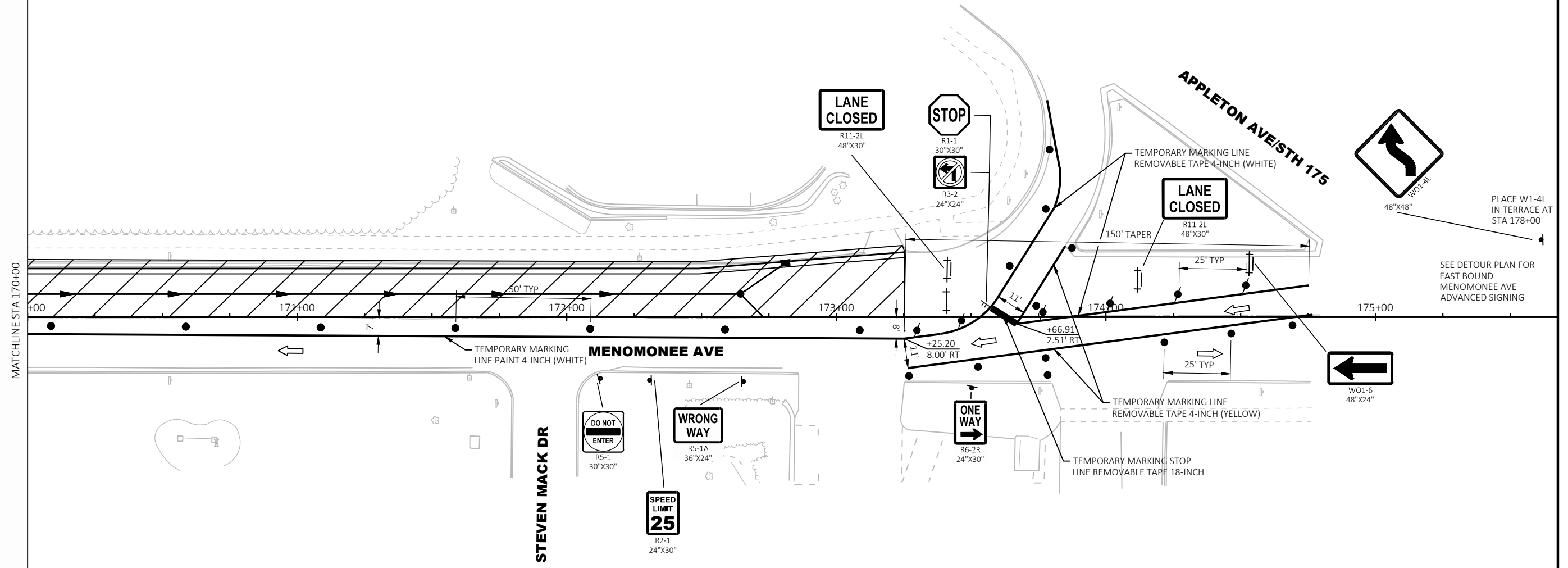
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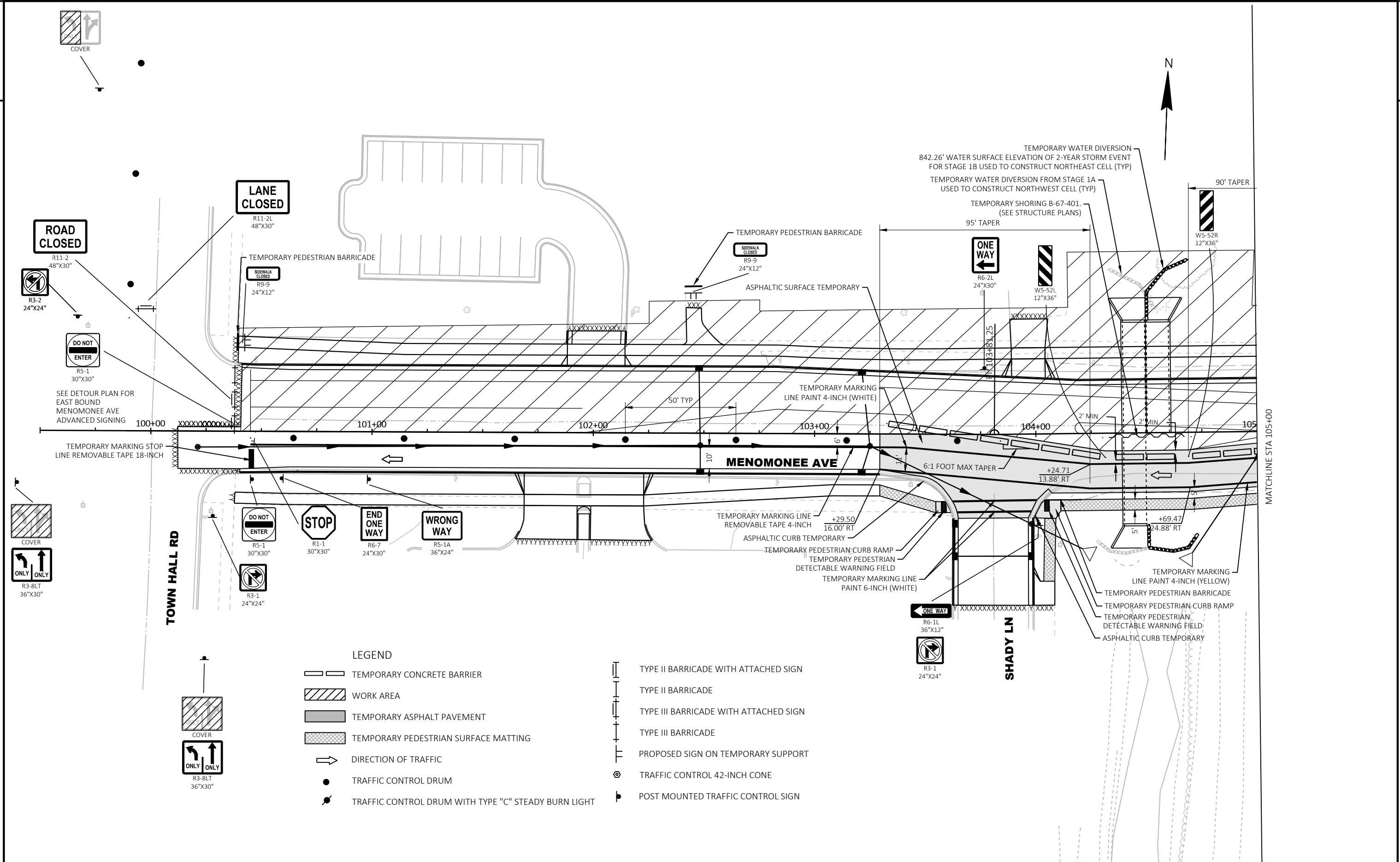


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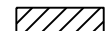














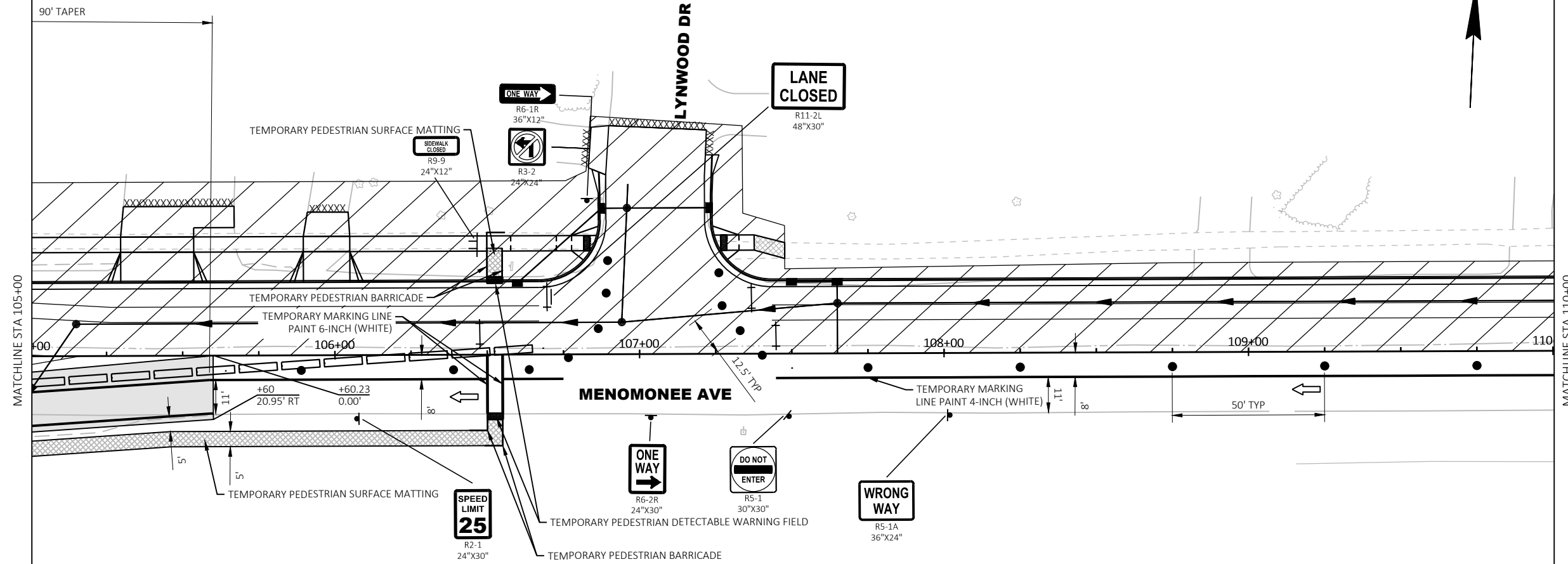


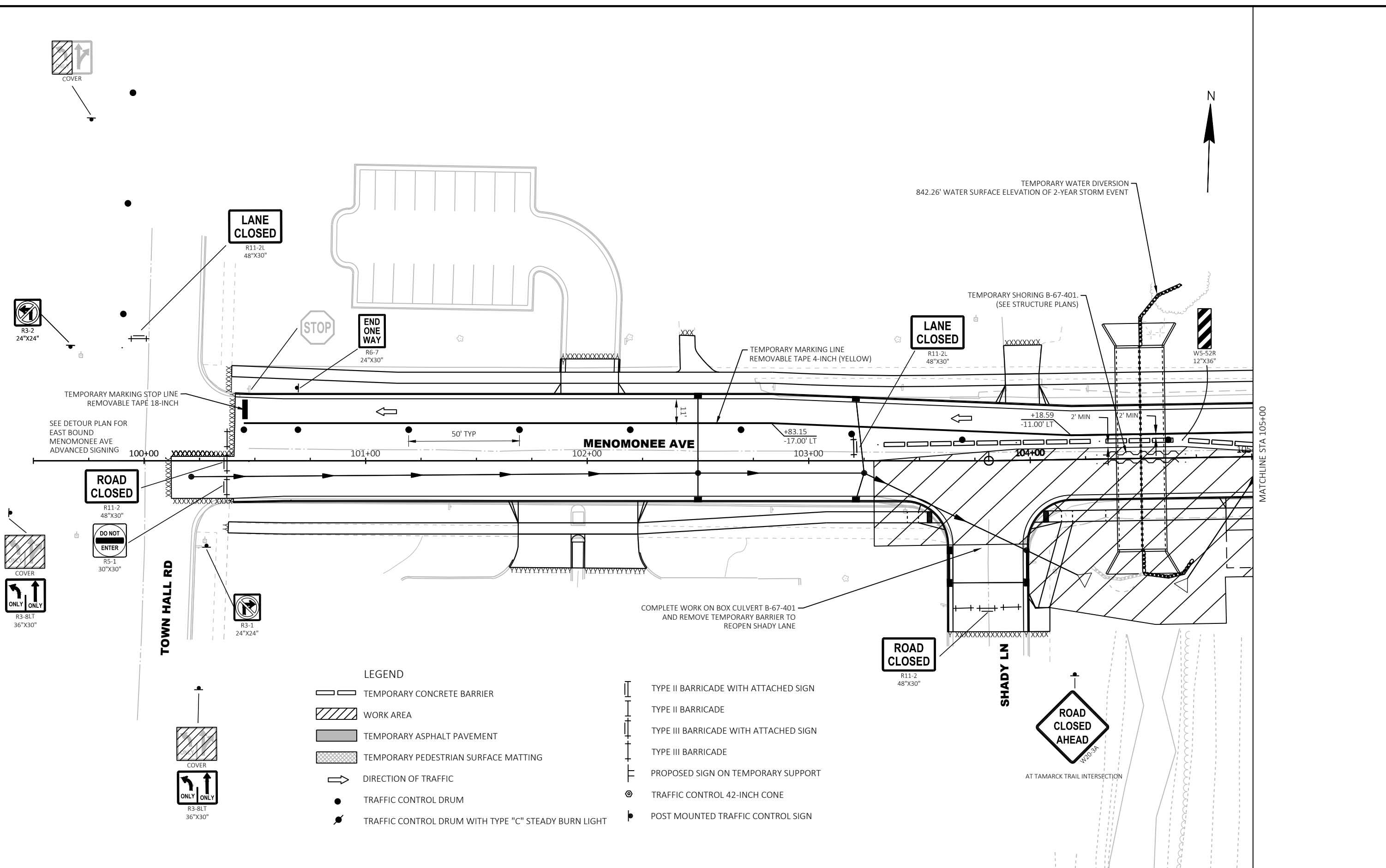
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


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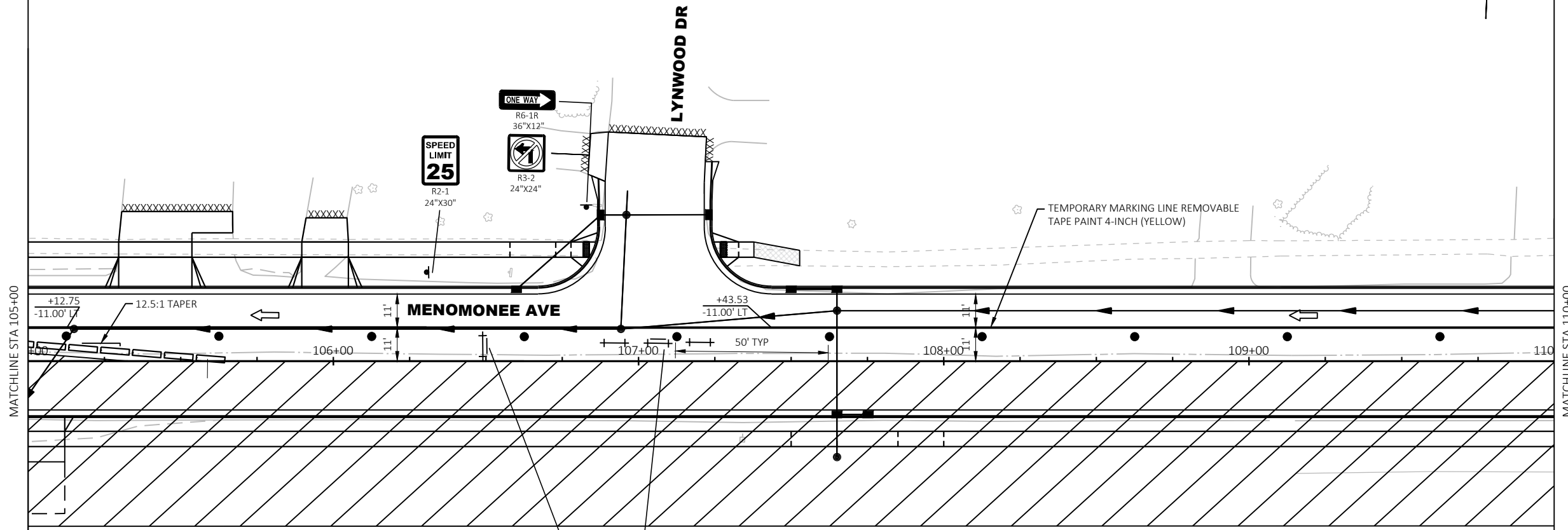
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


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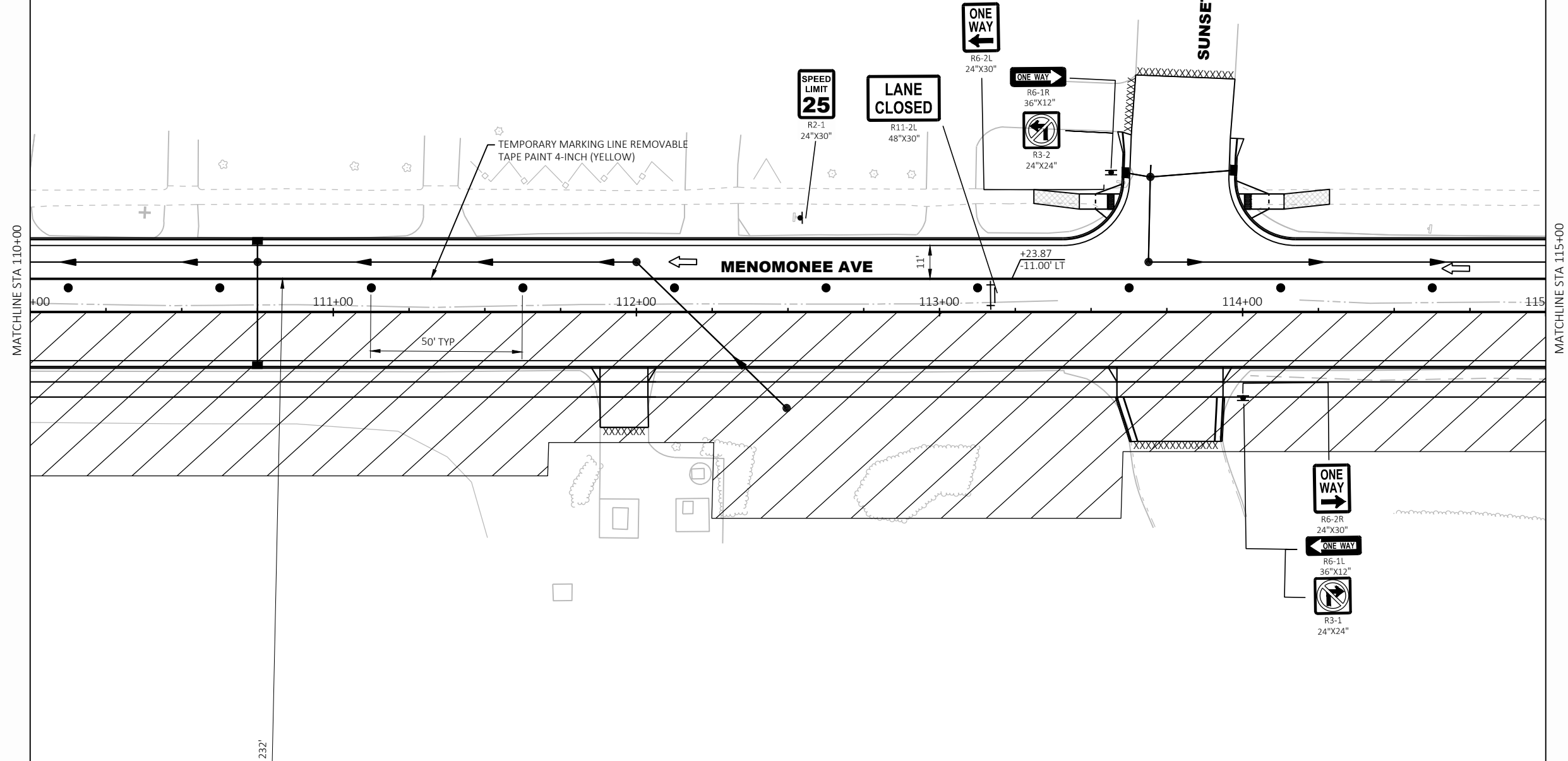
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















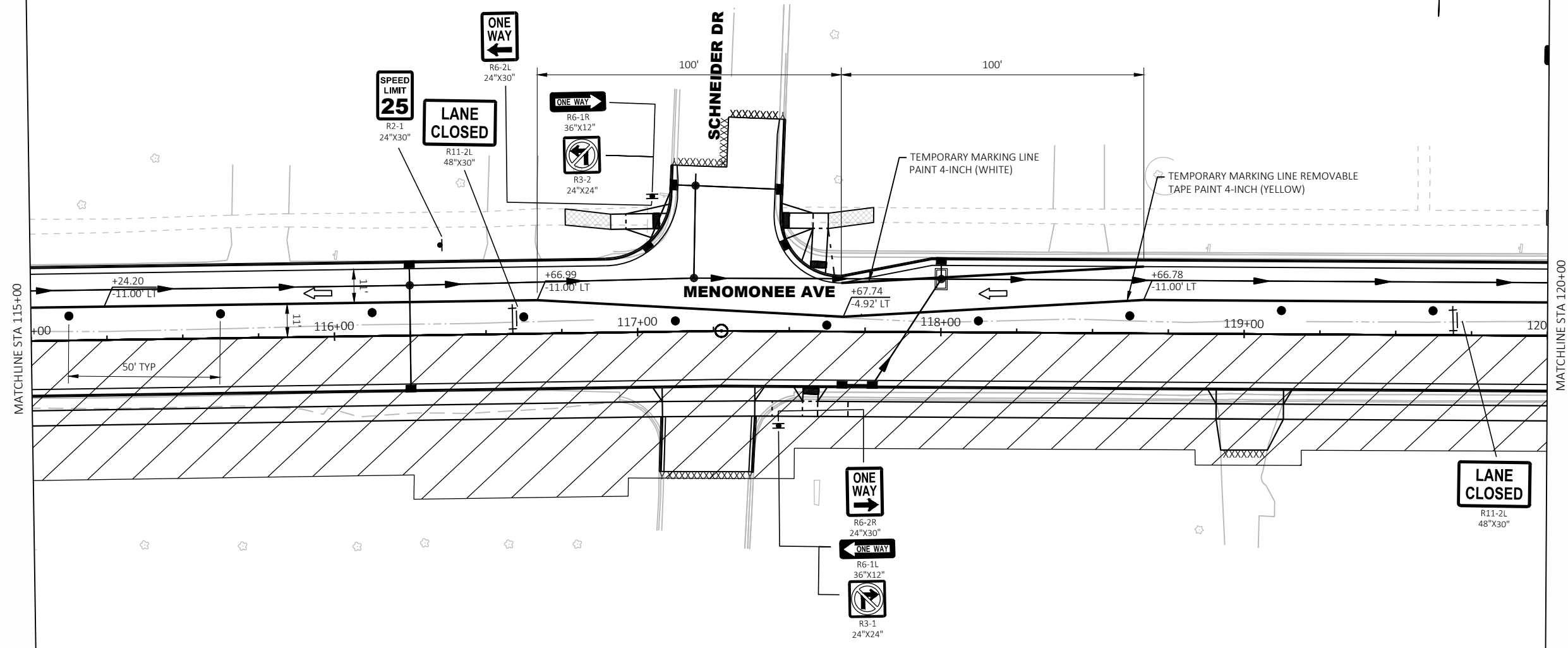
LANE CLOSED
R11-2L
48"x30"

ONE WAY
R6-2R
24"x30"















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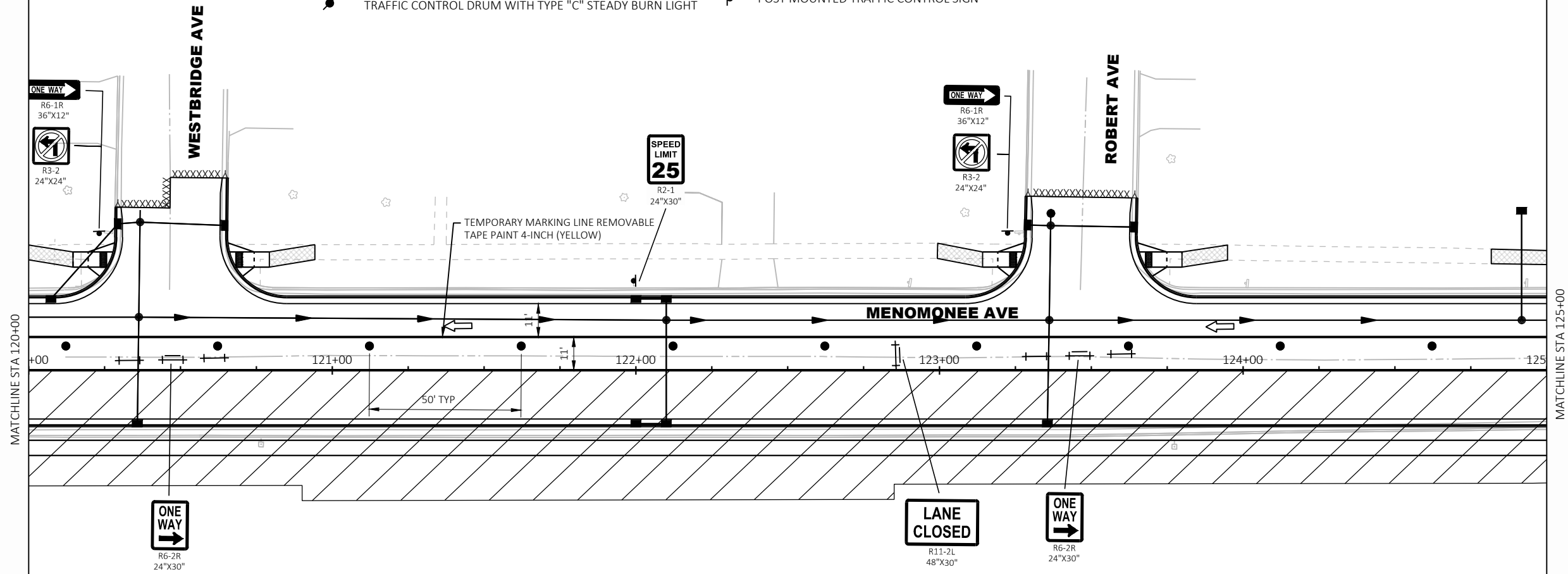
















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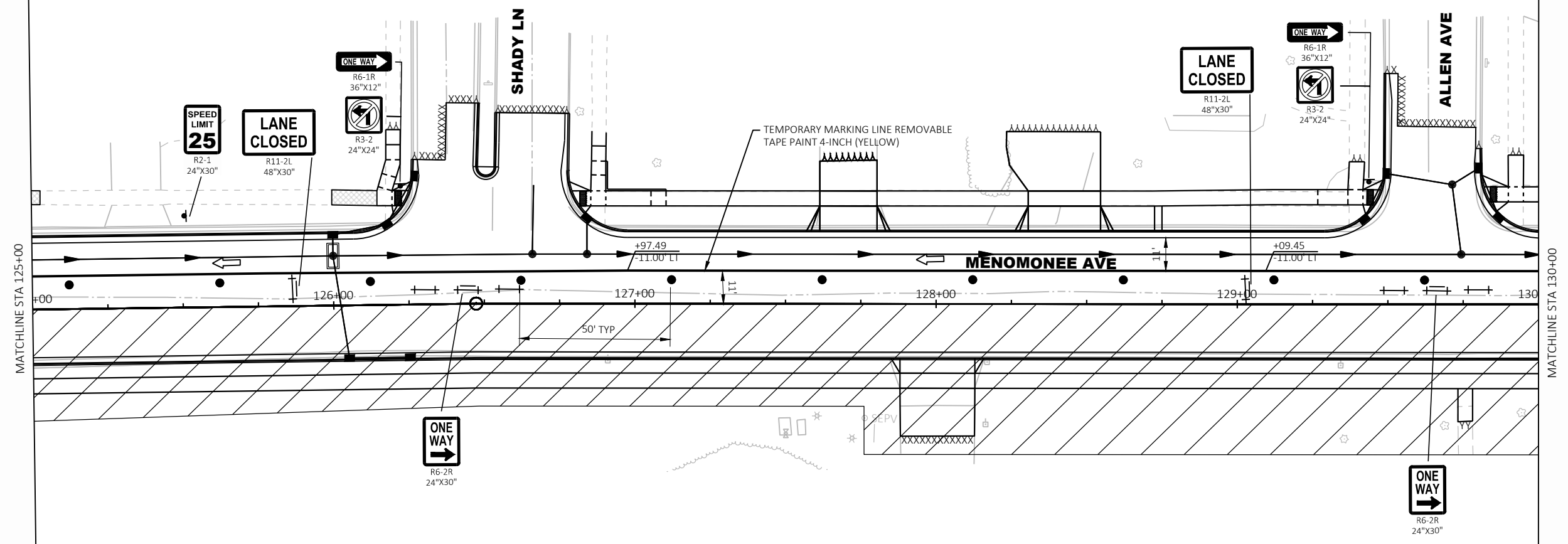
















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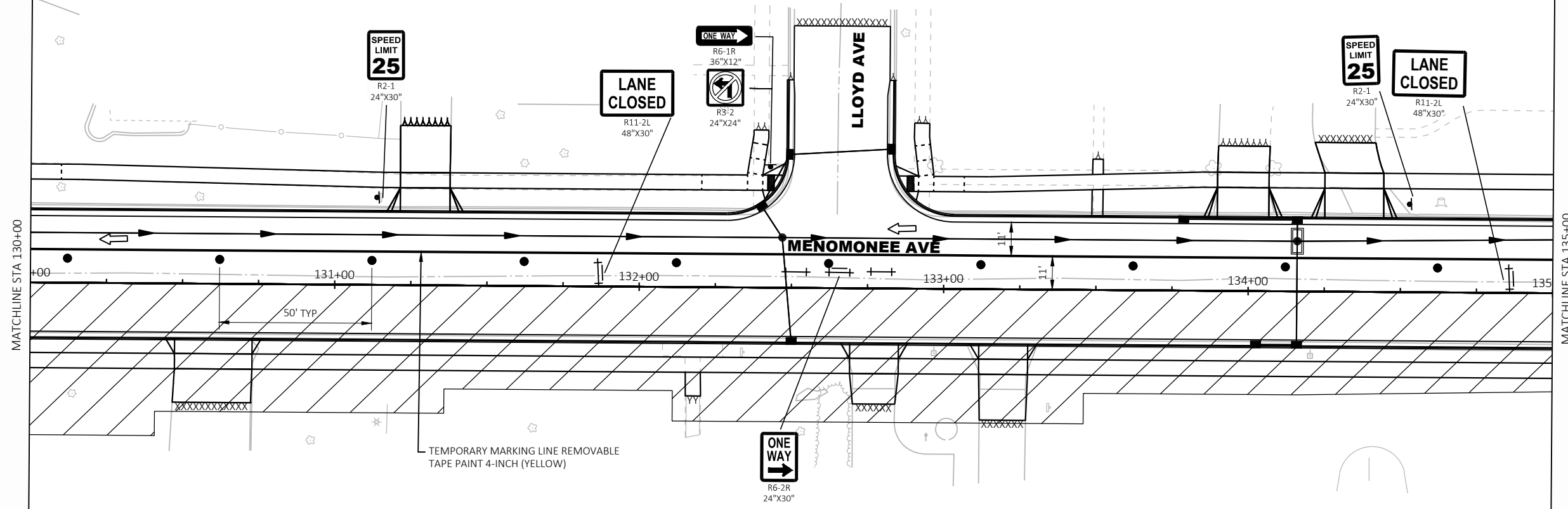
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













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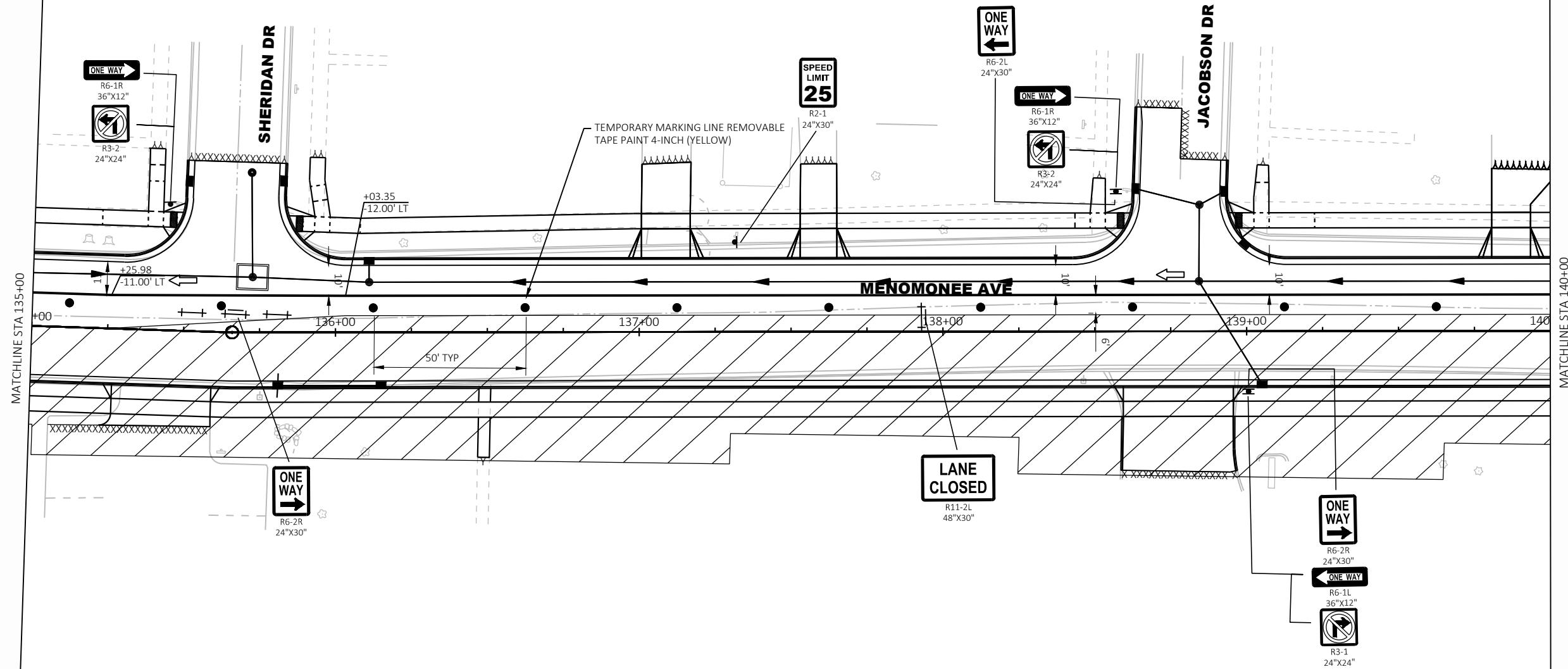


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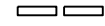















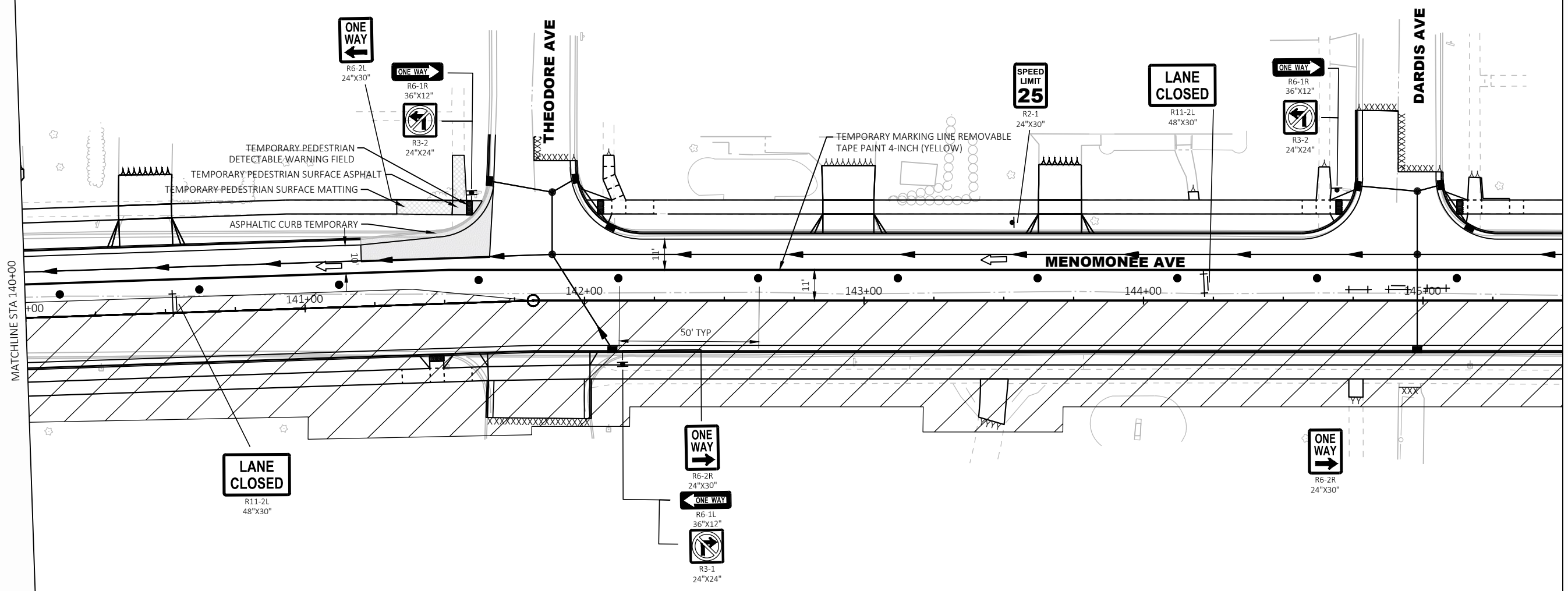
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













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-  POST MOUNTED TRAFFIC CONTROL SIGN

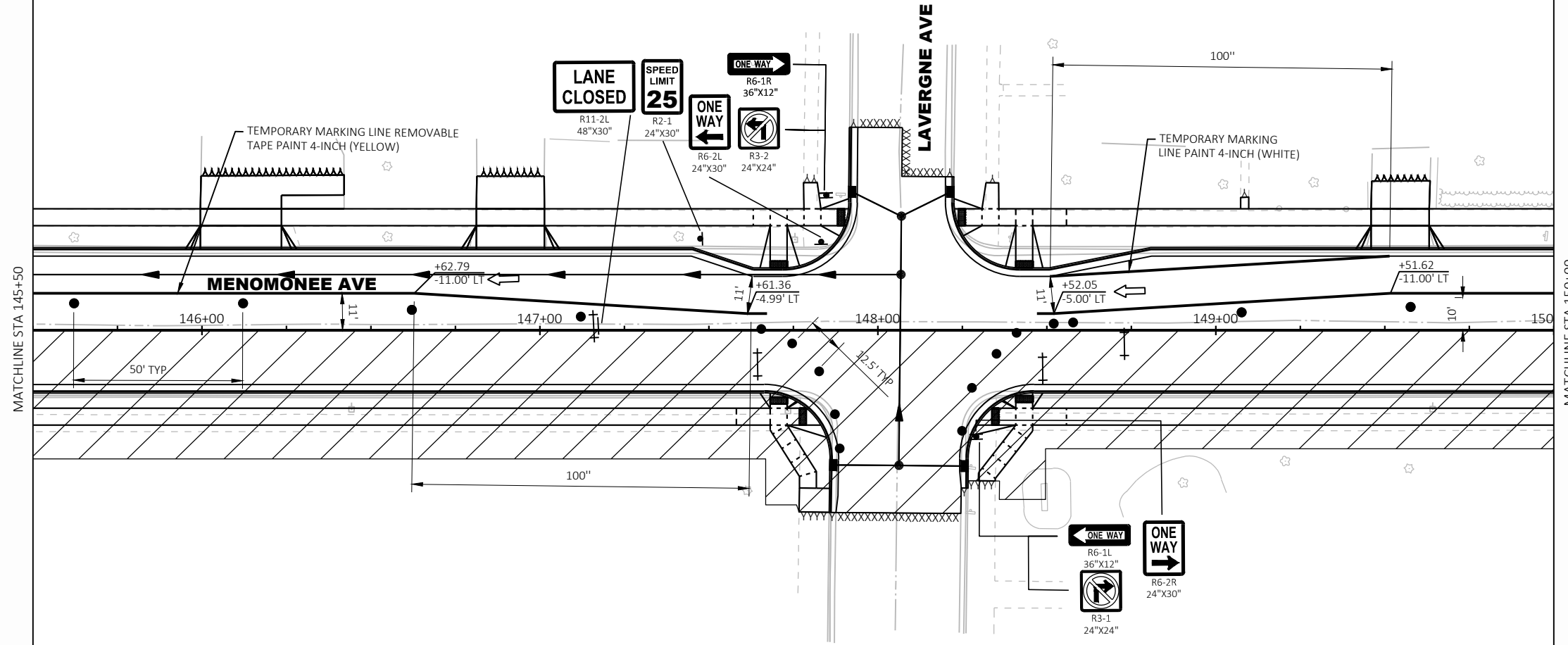
















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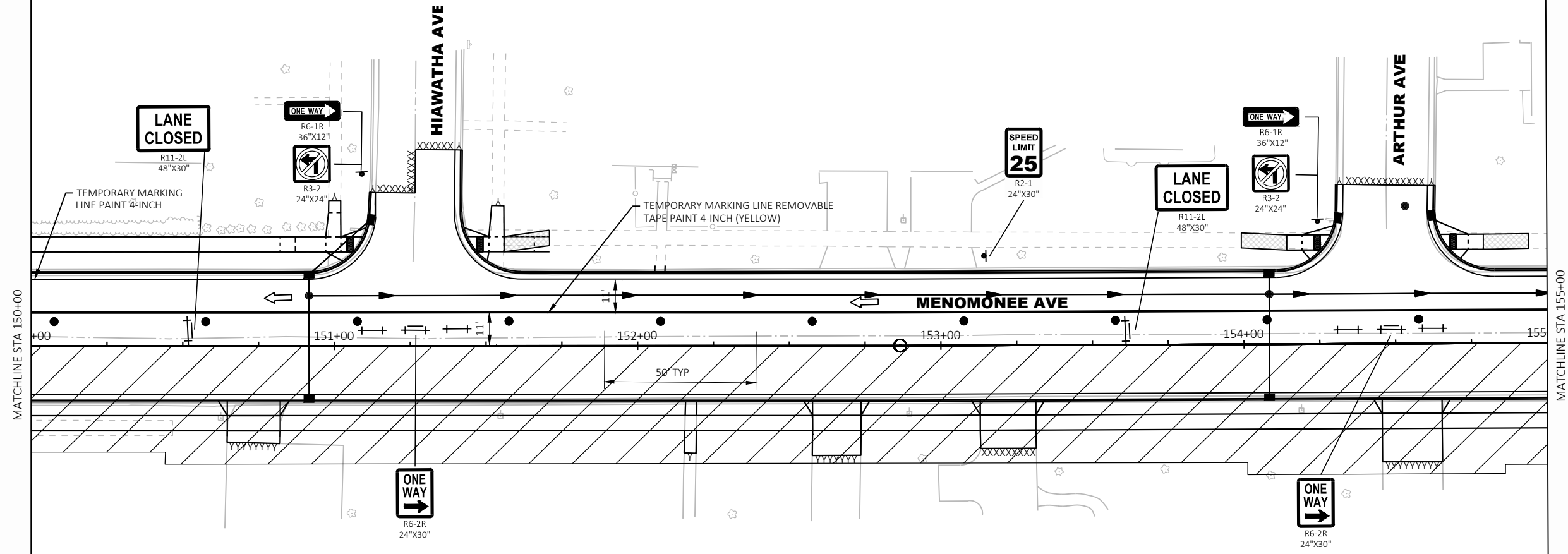
-  TEMPORARY CONCRETE BARRIER
-  WORK AREA
-  TEMPORARY ASPHALT PAVEMENT
-  TEMPORARY PEDESTRIAN SURFACE MATTING
-  DIRECTION OF TRAFFIC
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
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








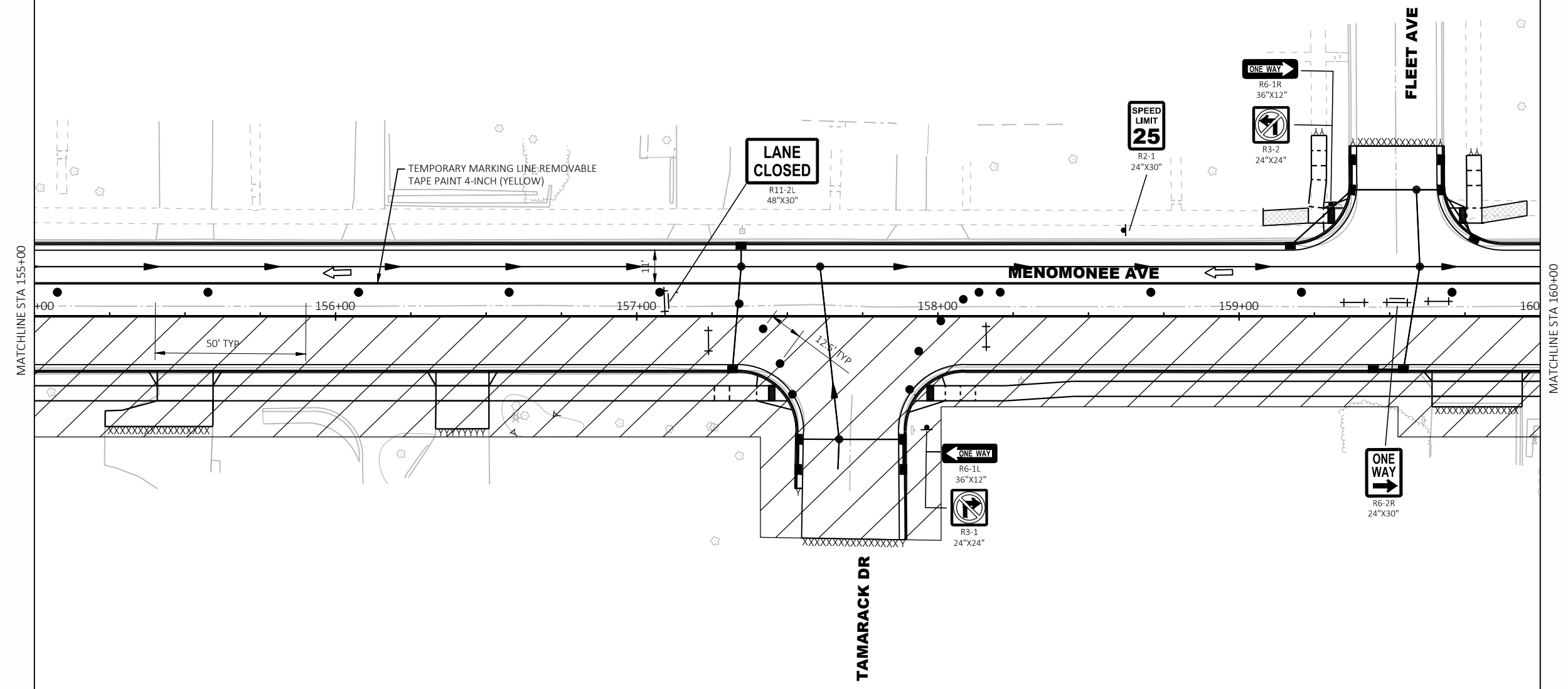
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




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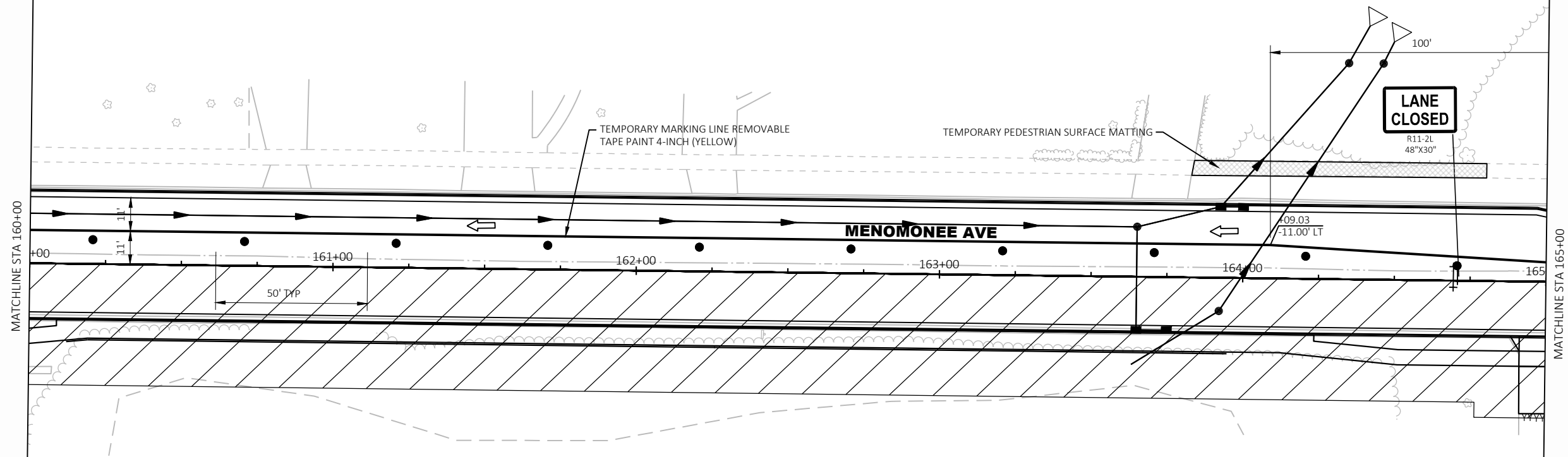
















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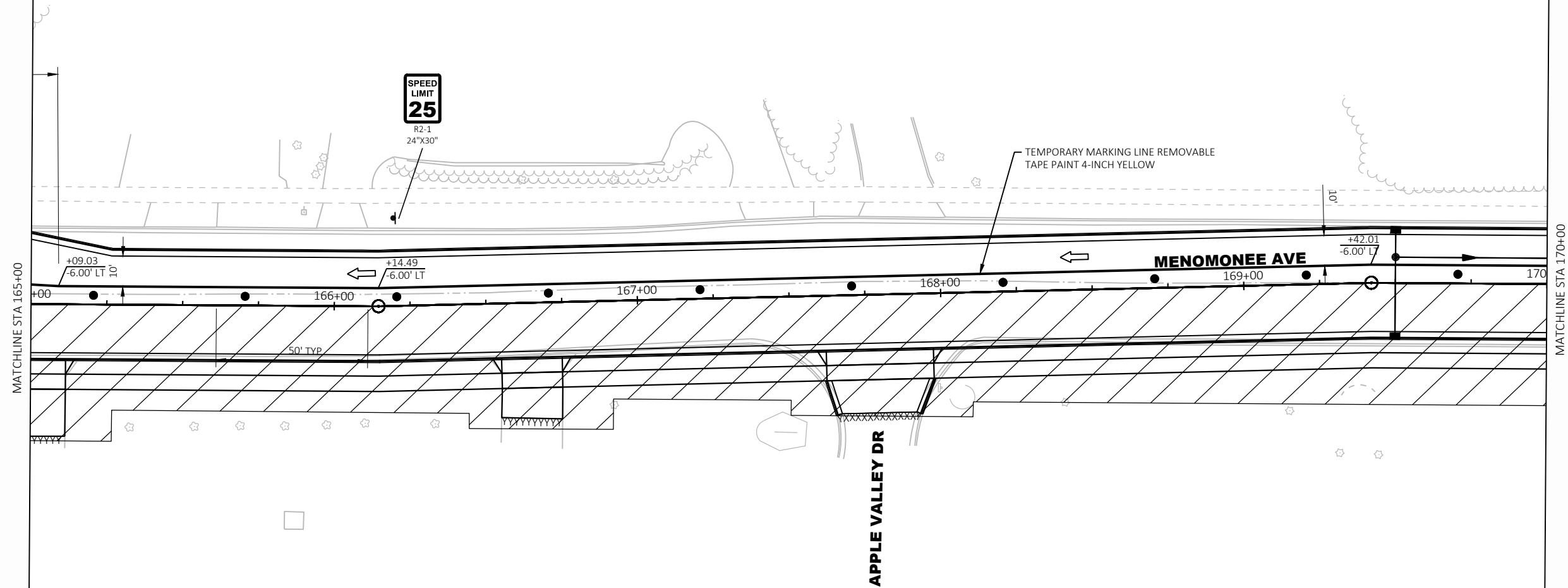
















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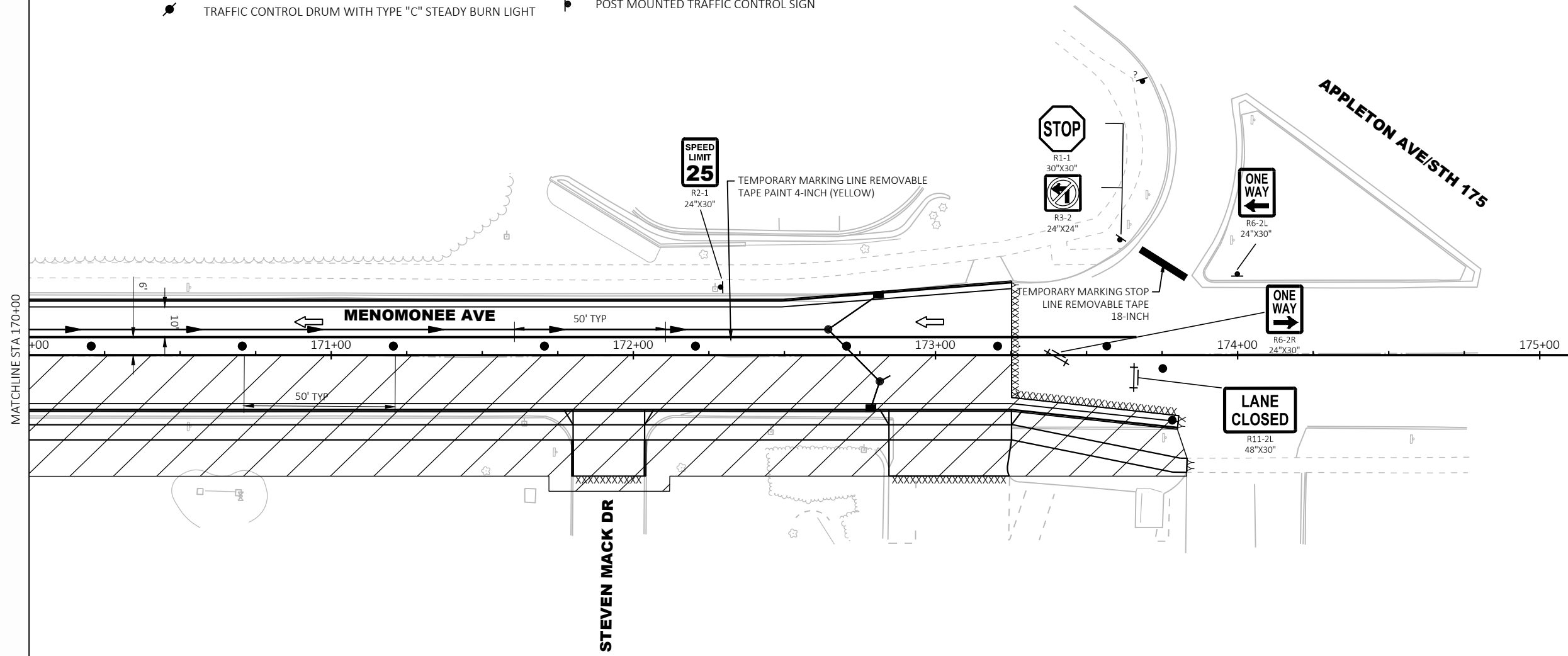
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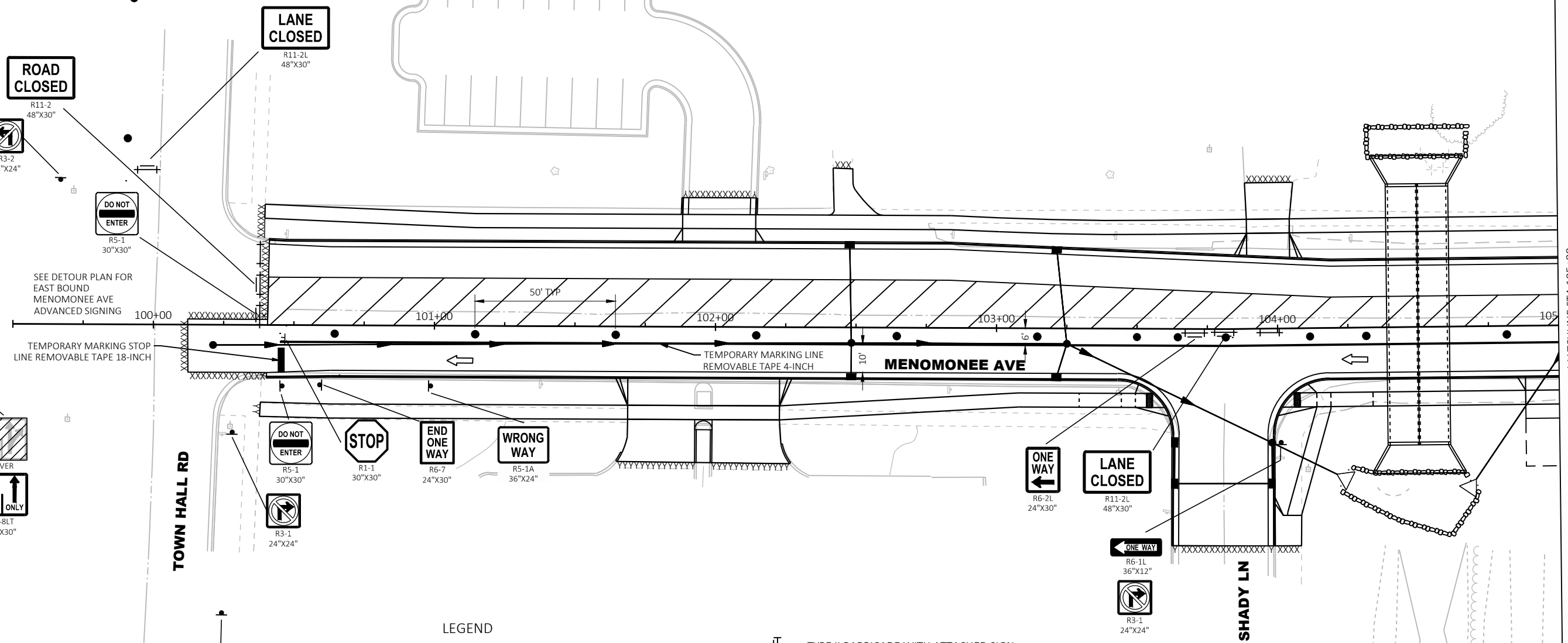
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SEE DETOUR PLAN FOR EAST BOUND MENOMONEE AVE ADVANCED SIGNING



TOWN HALL RD

MENOMONEE AVE

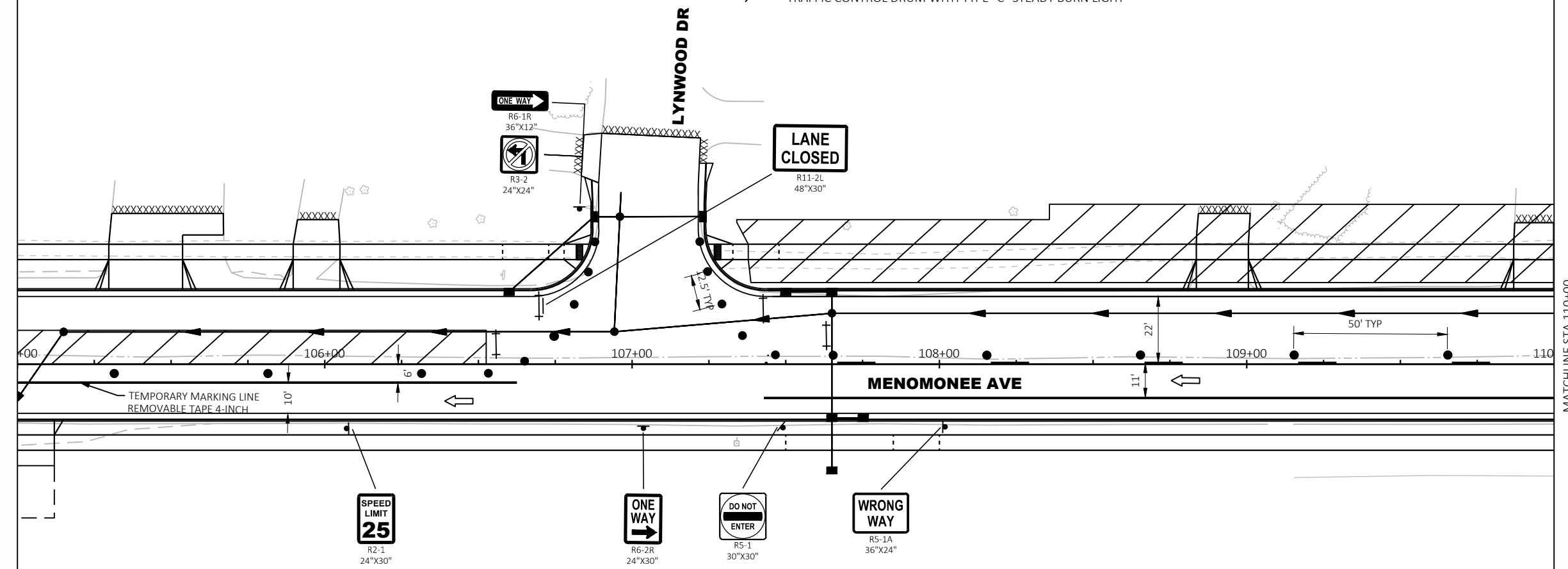
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MATCHLINE STA 105+00















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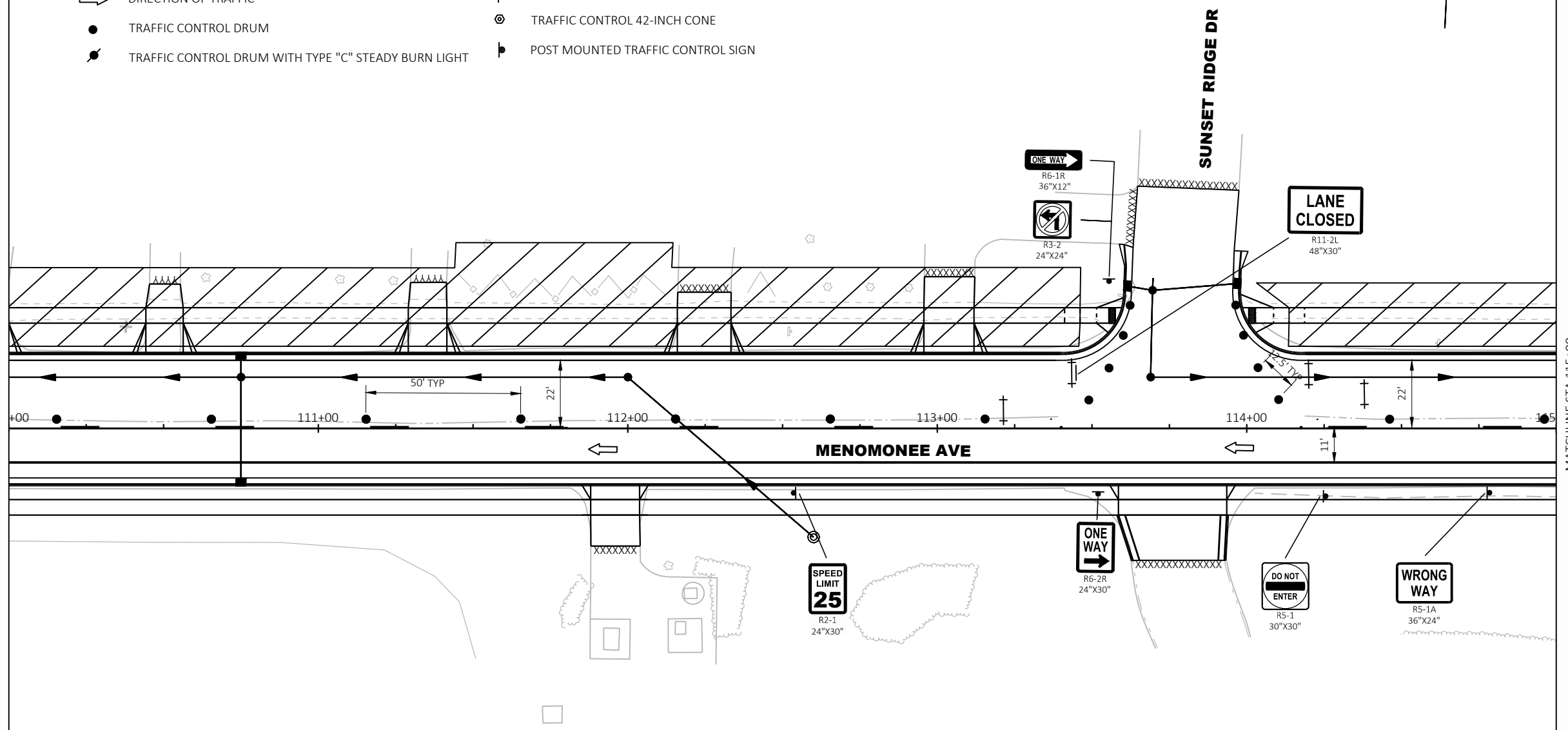
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






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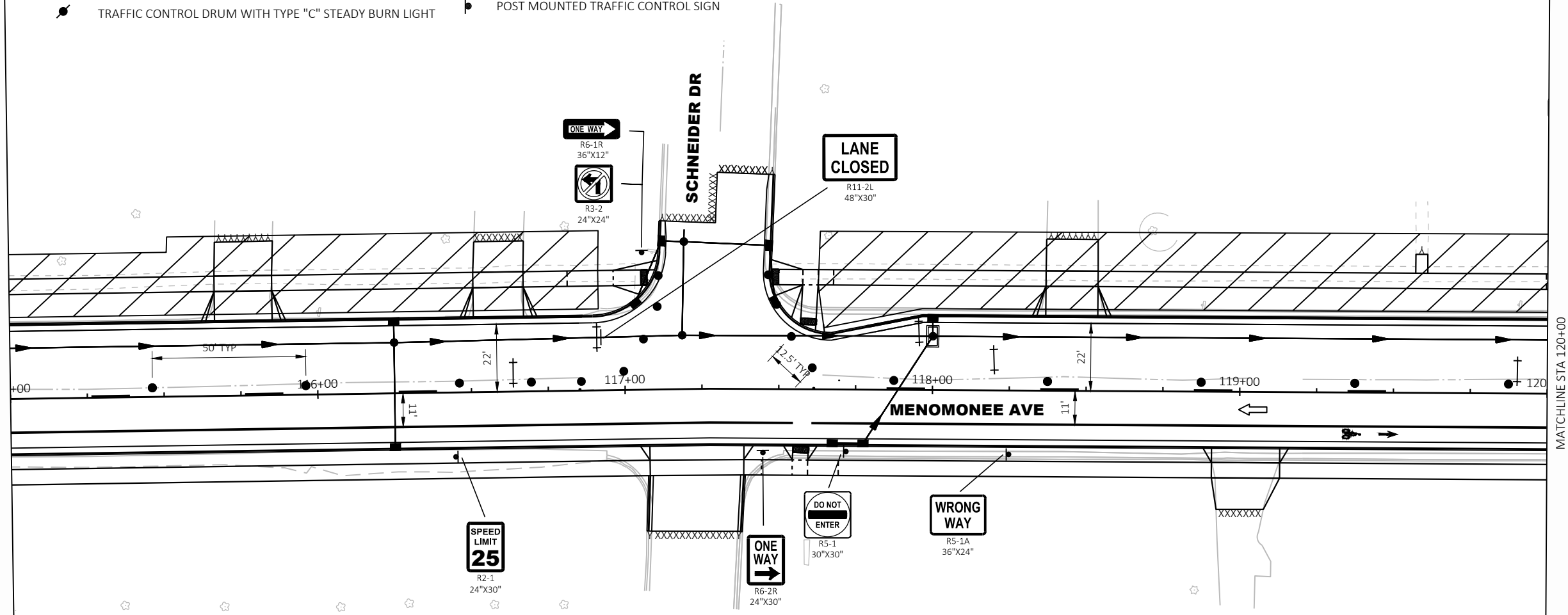


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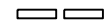












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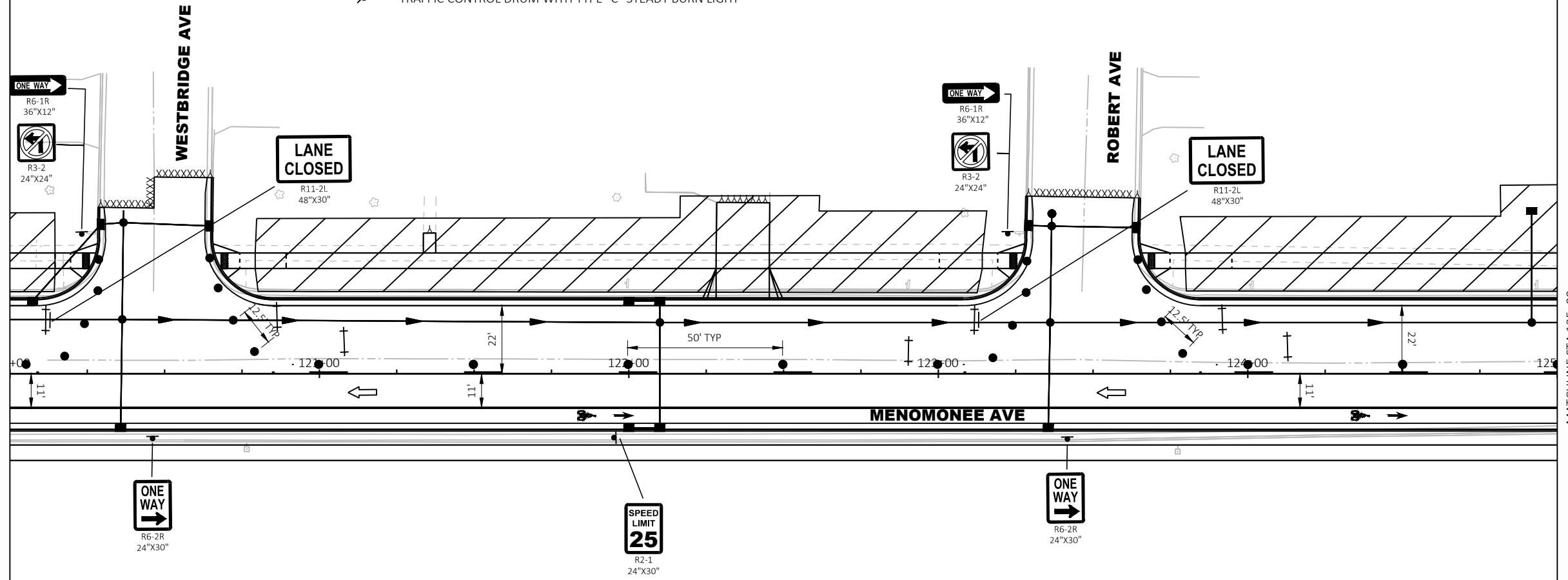


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











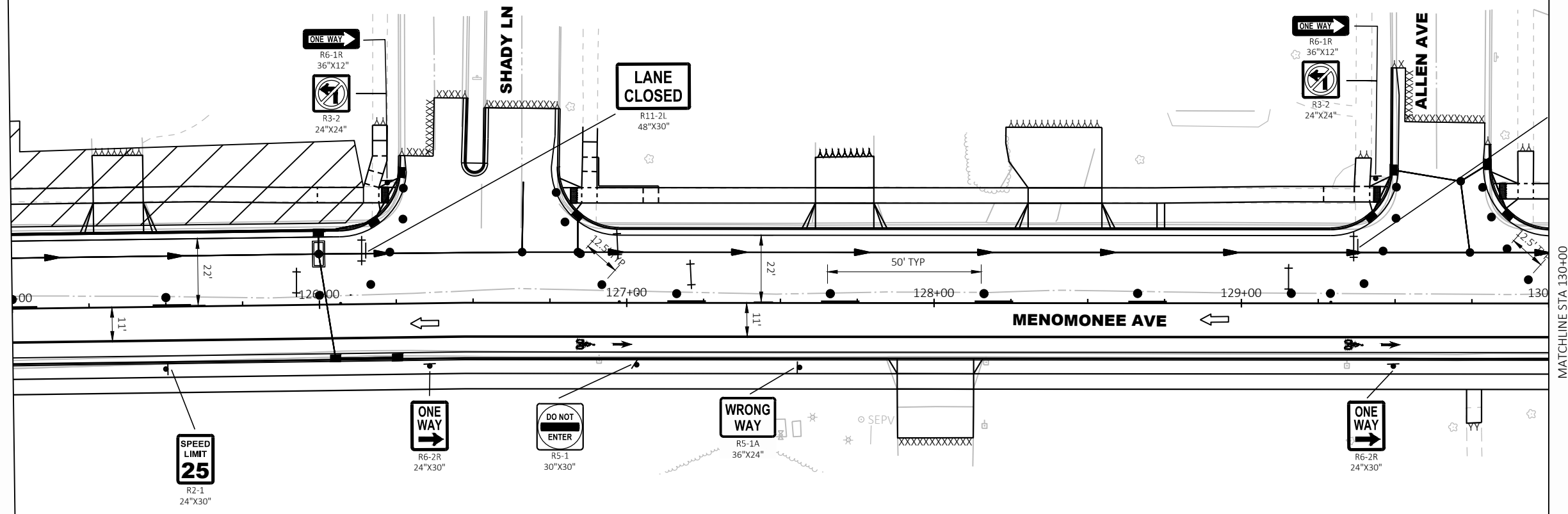
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



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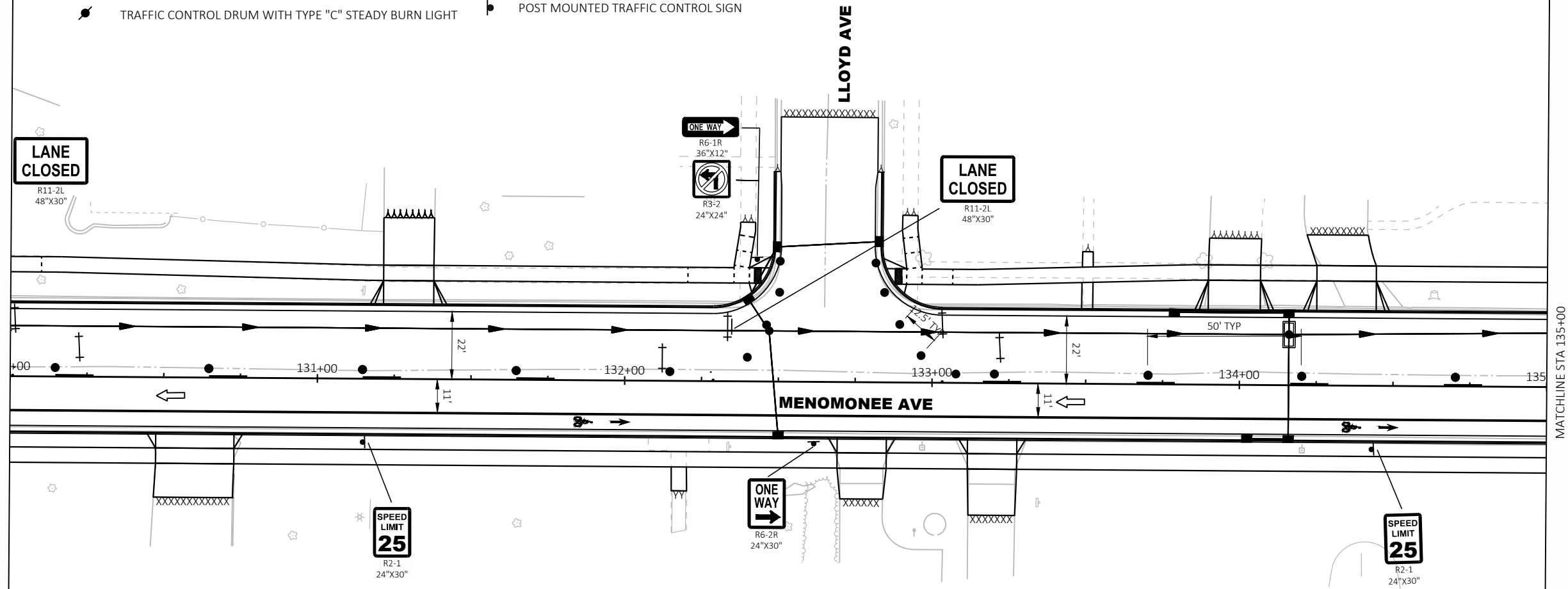


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













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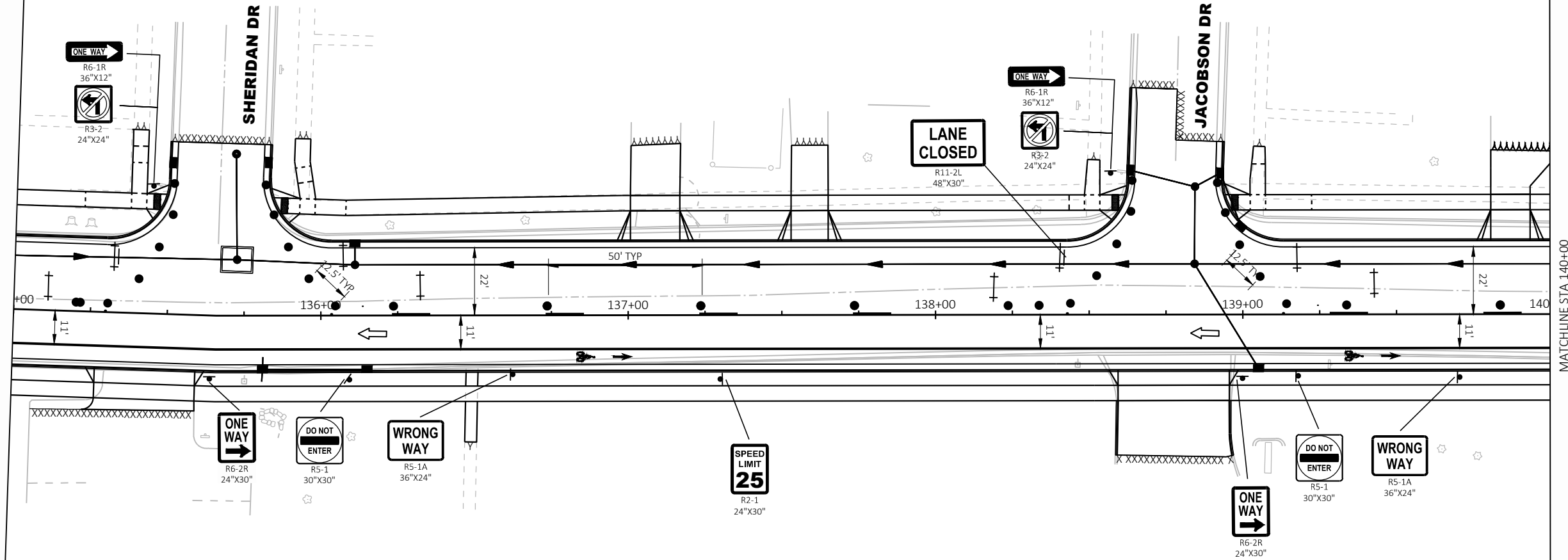


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















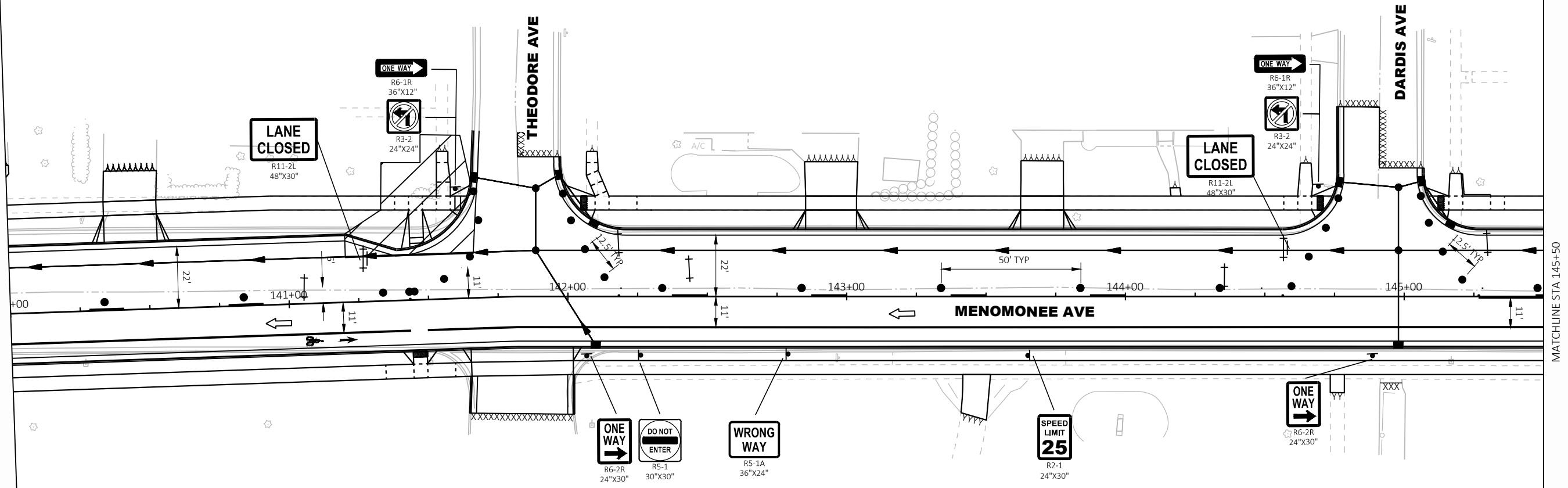
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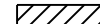














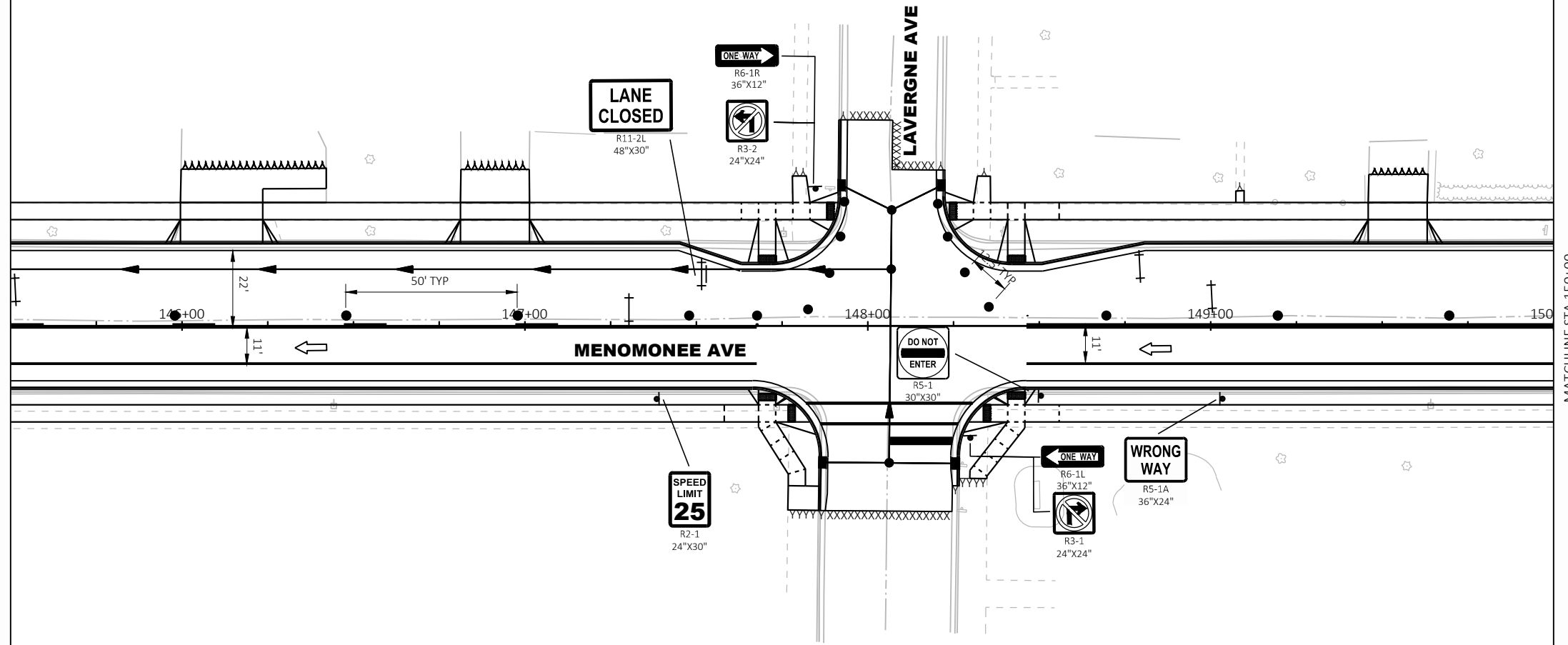
MATCHLINE STA 140+00

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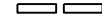















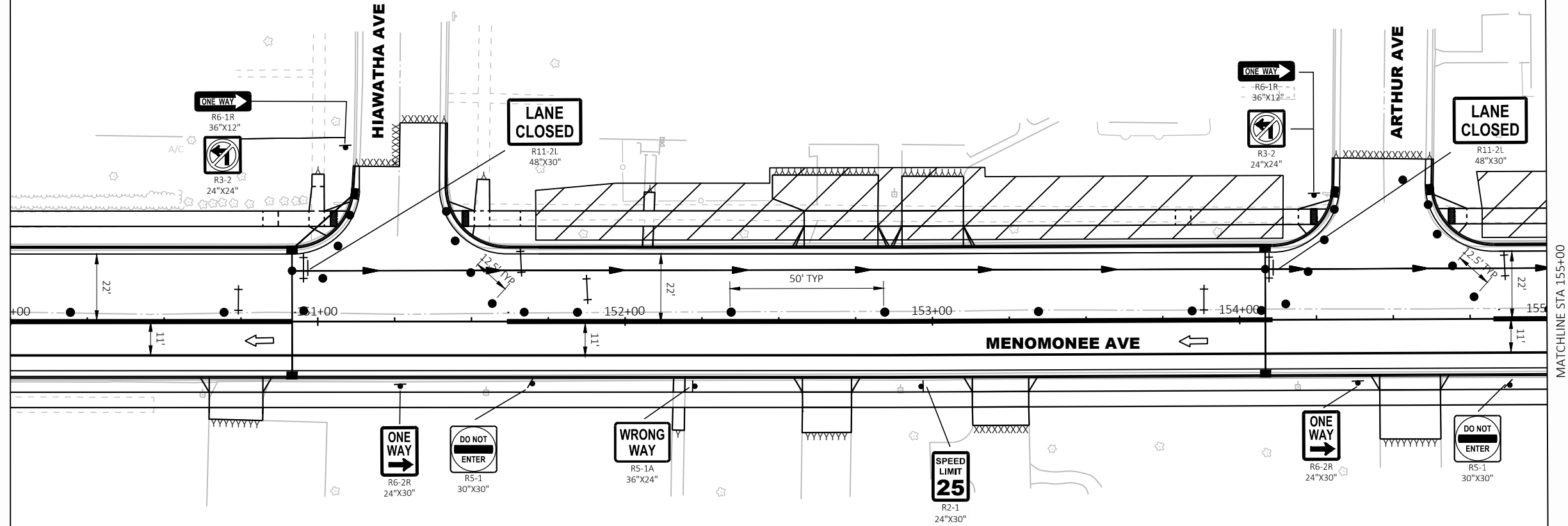
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













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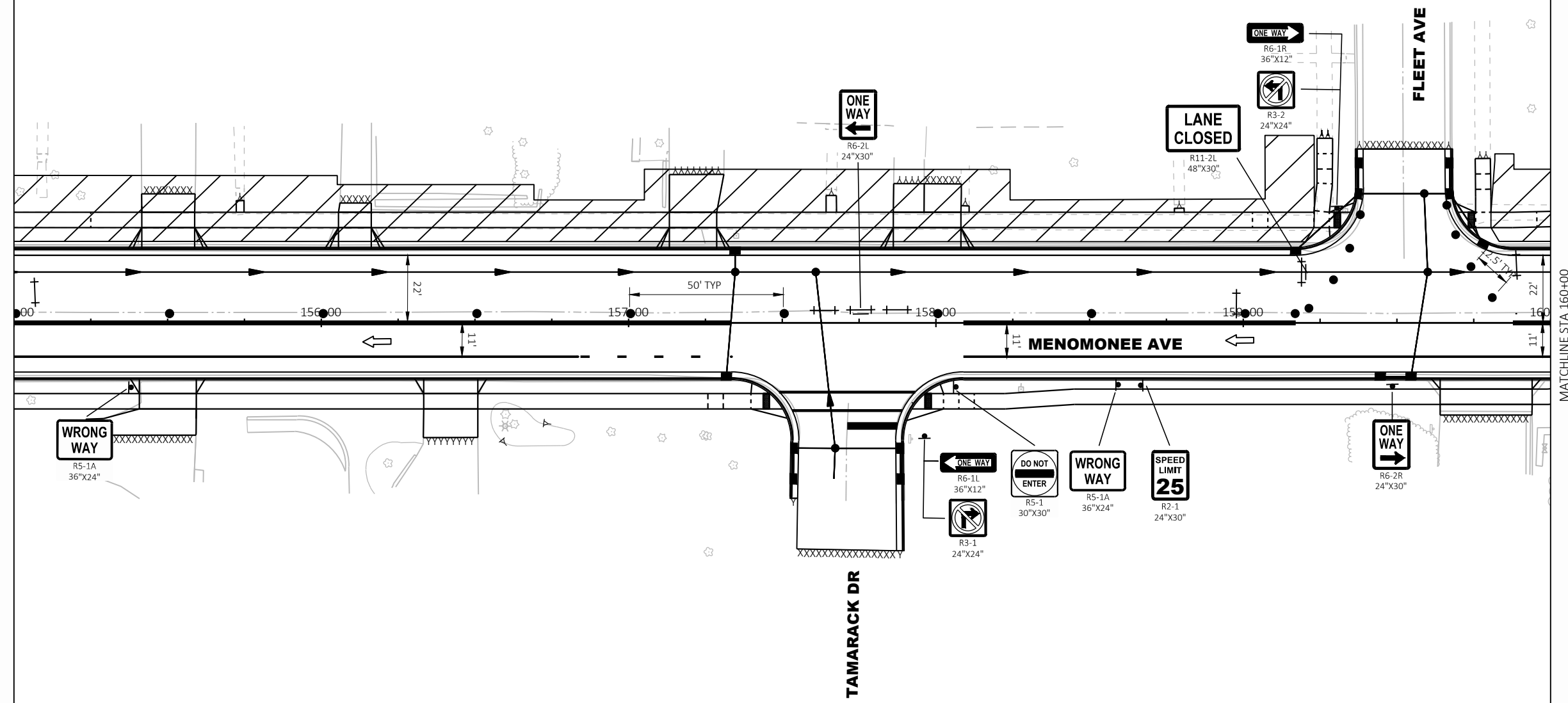


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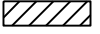












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-  TEMPORARY PEDESTRIAN SURFACE MATTING
-  DIRECTION OF TRAFFIC
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
-  TYPE II BARRICADE WITH ATTACHED SIGN
-  TYPE II BARRICADE
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  TYPE III BARRICADE
-  PROPOSED SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL 42-INCH CONE
-  POST MOUNTED TRAFFIC CONTROL SIGN

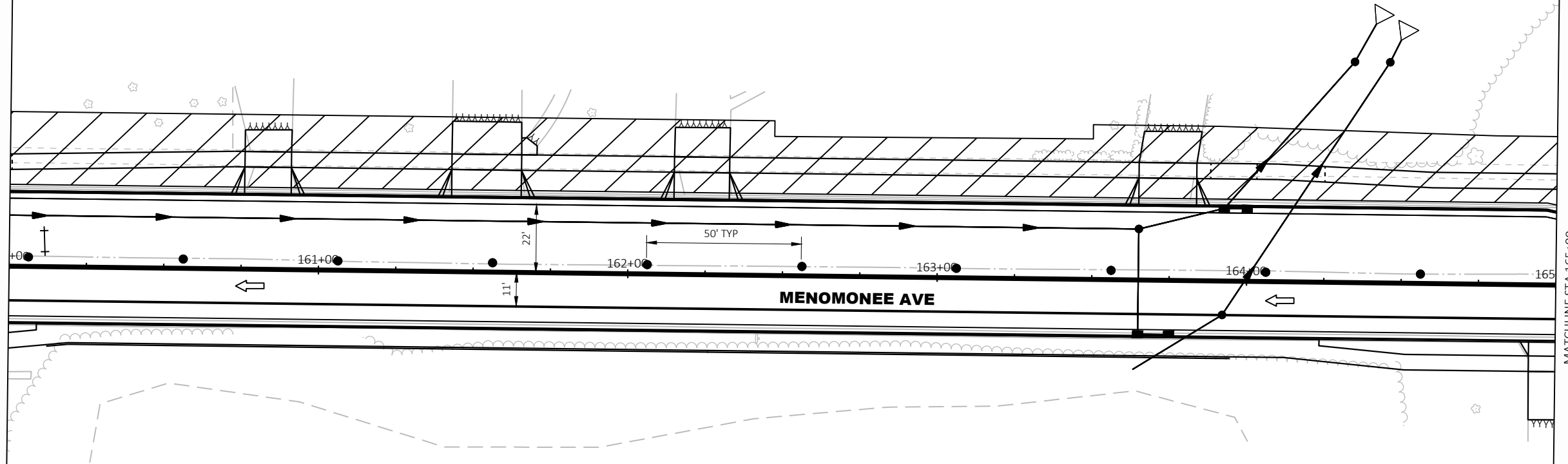
















- LEGEND**
-  TEMPORARY CONCRETE BARRIER
 -  WORK AREA
 -  TEMPORARY ASPHALT PAVEMENT
 -  TEMPORARY PEDESTRIAN SURFACE MATTING
 -  DIRECTION OF TRAFFIC
 -  TRAFFIC CONTROL DRUM
 -  TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
 -  TYPE II BARRICADE WITH ATTACHED SIGN
 -  TYPE II BARRICADE
 -  TYPE III BARRICADE WITH ATTACHED SIGN
 -  TYPE III BARRICADE
 -  PROPOSED SIGN ON TEMPORARY SUPPORT
 -  TRAFFIC CONTROL 42-INCH CONE
 -  POST MOUNTED TRAFFIC CONTROL SIGN

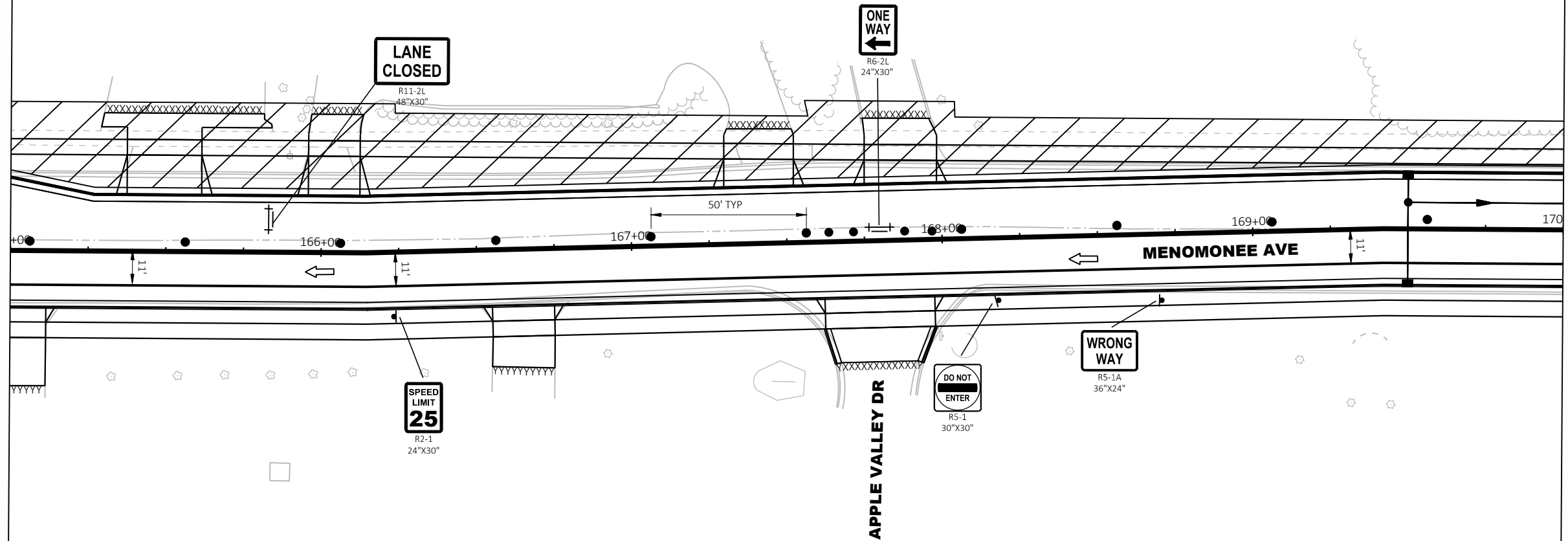


MATCHLINE STA 160+00















- LEGEND**
-  TEMPORARY CONCRETE BARRIER
 -  WORK AREA
 -  TEMPORARY ASPHALT PAVEMENT
 -  TEMPORARY PEDESTRIAN SURFACE MATTING
 -  DIRECTION OF TRAFFIC
 -  TRAFFIC CONTROL DRUM
 -  TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
 -  TYPE II BARRICADE WITH ATTACHED SIGN
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 -  PROPOSED SIGN ON TEMPORARY SUPPORT
 -  TRAFFIC CONTROL 42-INCH CONE
 -  POST MOUNTED TRAFFIC CONTROL SIGN

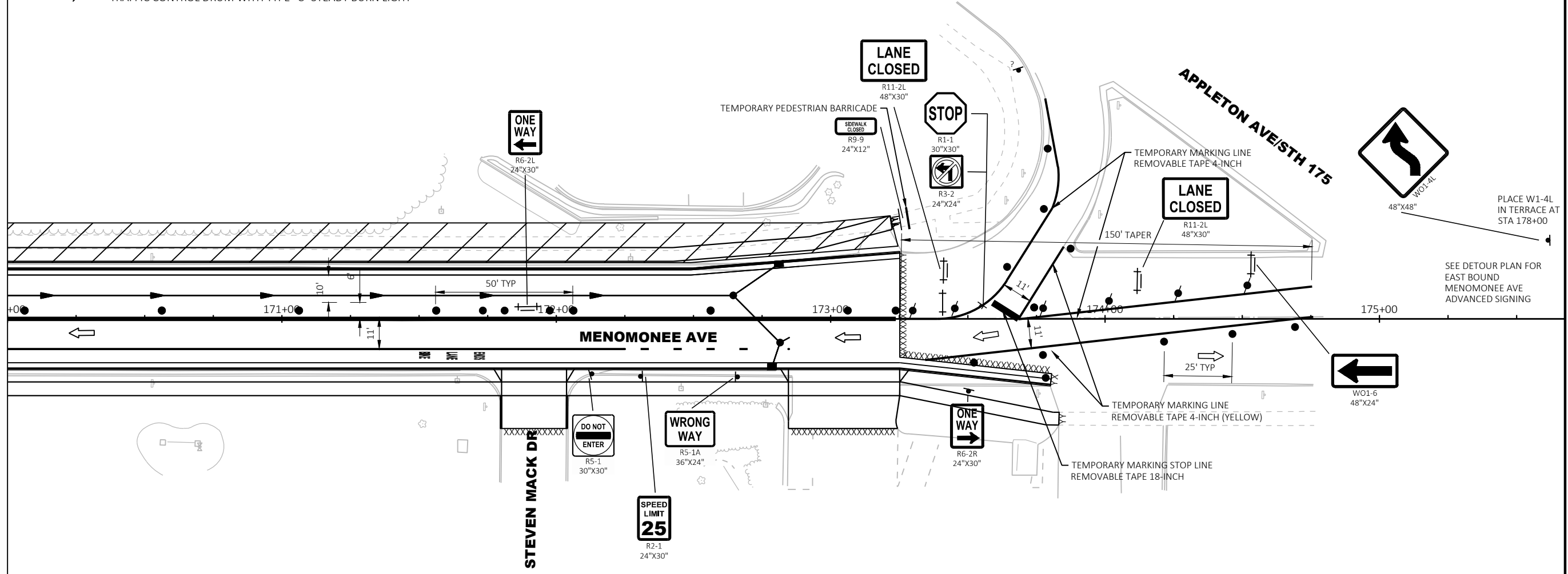



- LEGEND**
-  TEMPORARY CONCRETE BARRIER
 -  WORK AREA
 -  TEMPORARY ASPHALT PAVEMENT
 -  TEMPORARY PEDESTRIAN SURFACE MATTING
 -  DIRECTION OF TRAFFIC
 -  TRAFFIC CONTROL DRUM
 -  TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
 -  TYPE II BARRICADE WITH ATTACHED SIGN
 -  TYPE II BARRICADE
 -  TYPE III BARRICADE WITH ATTACHED SIGN
 -  TYPE III BARRICADE
 -  PROPOSED SIGN ON TEMPORARY SUPPORT
 -  TRAFFIC CONTROL 42-INCH CONE
 -  POST MOUNTED TRAFFIC CONTROL SIGN





MATCHLINE STA 170+00


- LEGEND**
-  TEMPORARY CONCRETE BARRIER
 -  WORK AREA
 -  TEMPORARY ASPHALT PAVEMENT
 -  TEMPORARY PEDESTRIAN SURFACE MATTING
 -  DIRECTION OF TRAFFIC
 -  TRAFFIC CONTROL DRUM
 -  TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
 -  TYPE II BARRICADE WITH ATTACHED SIGN
 -  TYPE II BARRICADE
 -  TYPE III BARRICADE WITH ATTACHED SIGN
 -  TYPE III BARRICADE
 -  PROPOSED SIGN ON TEMPORARY SUPPORT
 -  TRAFFIC CONTROL 42-INCH CONE
 -  POST MOUNTED TRAFFIC CONTROL SIGN





1

 M4-9BR
 30"x24"

2

 M4-9BL
 30"x24"







3

 R3-1
 24"x24"

4

 R11-2
 48"x30"

5

 W20-3A

6

 R9-9
 24"x12"

LEGEND








-  WORK ZONE
-  DIRECTION OF TRAFFIC
-  UNDER PEDESTRIAN TRAFFIC
-  PEDESTRIAN DETOUR ROUTE
-  SIGN MOUNTED ON POST
-  SIGN MOUNTED ON TEMPORARY SUPPORT

- NOTES:**
1. THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.
 2. ALL EXISTING SIGNS THAT NEED TO BE COVERED SHALL BE COVERED WITH A BLANK ORANGE PANEL.
 3. MAINTAIN ACCESS WITHIN WORK ZONE TO LOCAL TRAFFIC AND EMERGENCY VEHICLES AT ALL TIMES.
 4. SEE TRAFFIC CONTROL PLANS FOR ADDITIONAL SIGNS AND TRAFFIC CONTROL DEVICES.
 5. SEE STANDARD DETAIL DRAWINGS FOR LOCAL ROAD AND ADDITIONAL TRAFFIC CONTROL REQUIREMENTS.
 6. DO NOT CLOSE TWO CONSECUTIVE SIDE ROADS AT THE SAME TIME.





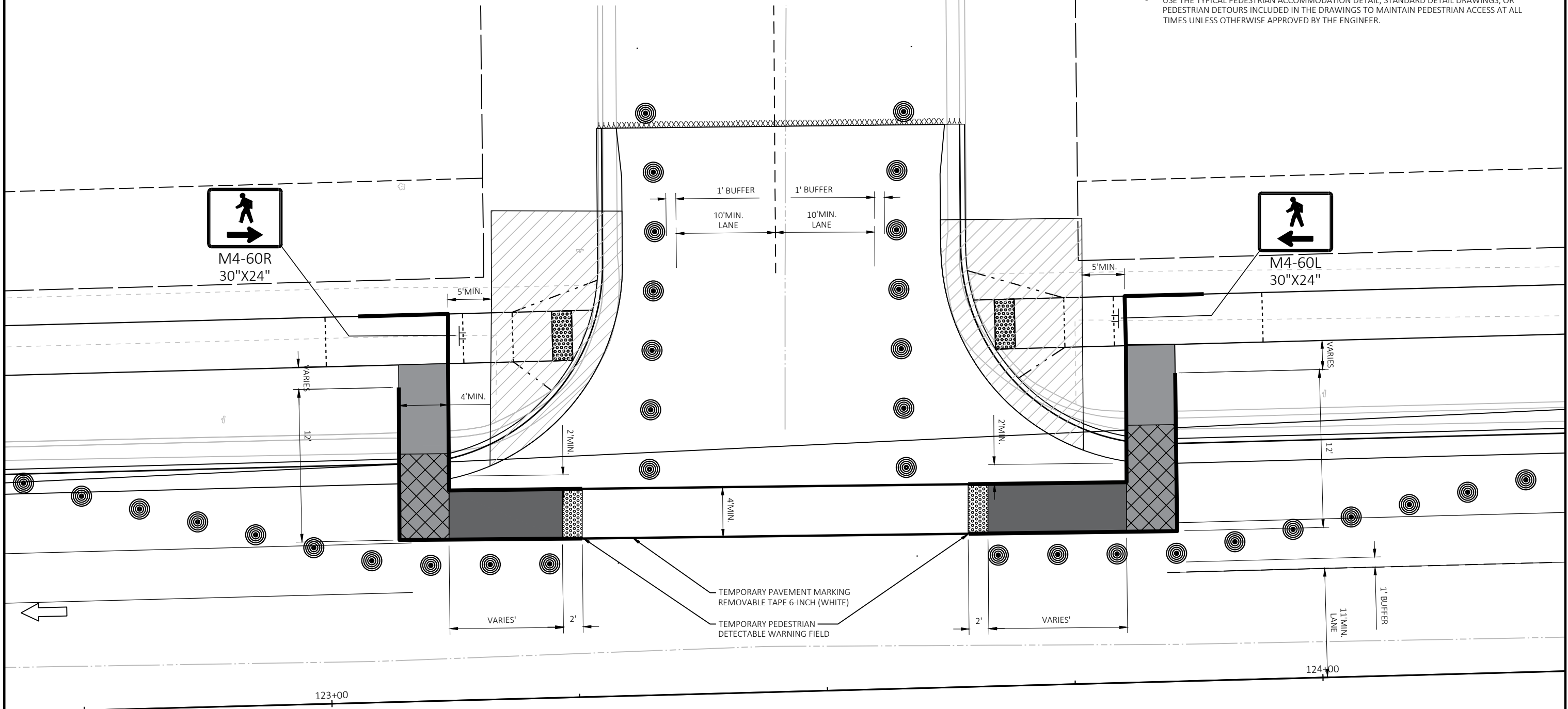
LEGEND

-  EXISTING SIGN MOUNTED ON POST(S)
-  EXISTING SIGN MOUNTED ON LIGHT POLE, SIGNAL POLE, OR OVERHEAD SUPPORT
-  SIGN NUMBER (REMOVING SIGNS TYPE II)
-  PROPOSED SIGN MOUNTED ON WOOD OR TABULAR STEEL POST(S)
-  PROPOSED SIGN MOUNTED ON LIGHT POLE, SIGNAL POLE, OR OVERHEAD SUPPORT
-  NEW SIGN NUMBER
-  UTILITY POLE

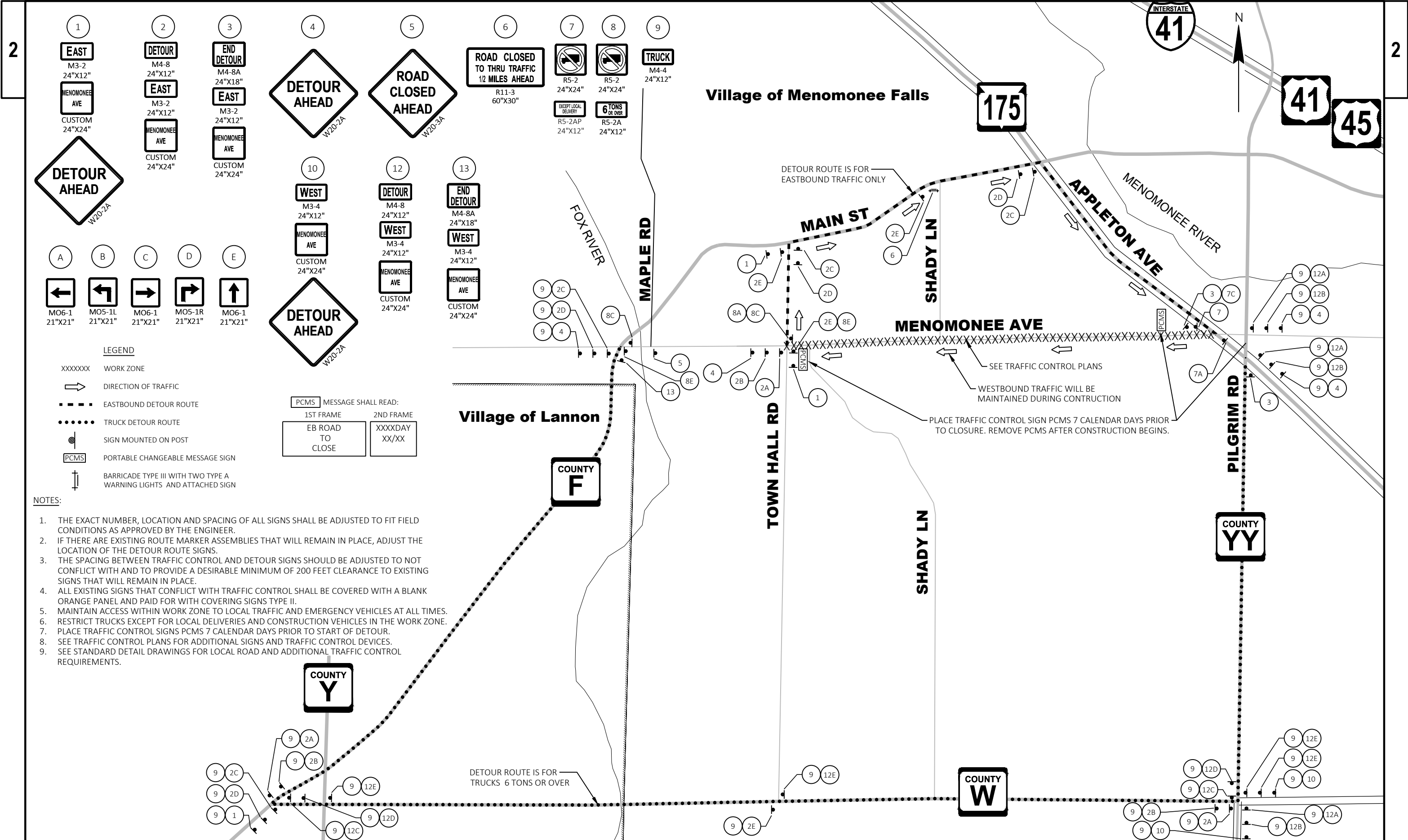
-  TRAFFIC CONTROL DRUM
-  WORK AREA
-  TEMPORARY PEDESTRIAN SURFACE PLATE
-  TEMPORARY PEDESTRIAN SURFACE MATTING
-  TEMPORARY CURB RAMP
-  SIGN ON TEMPORARY SUPPORT
-  TEMPORARY PEDESTRIAN BARRICADE

NOTES:

USE THE TYPICAL PEDESTRIAN ACCOMMODATION DETAIL, STANDARD DETAIL DRAWINGS, OR PEDESTRIAN DETOURS INCLUDED IN THE DRAWINGS TO MAINTAIN PEDESTRIAN ACCESS AT ALL TIMES UNLESS OTHERWISE APPROVED BY THE ENGINEER.



MENOMONEE AVE

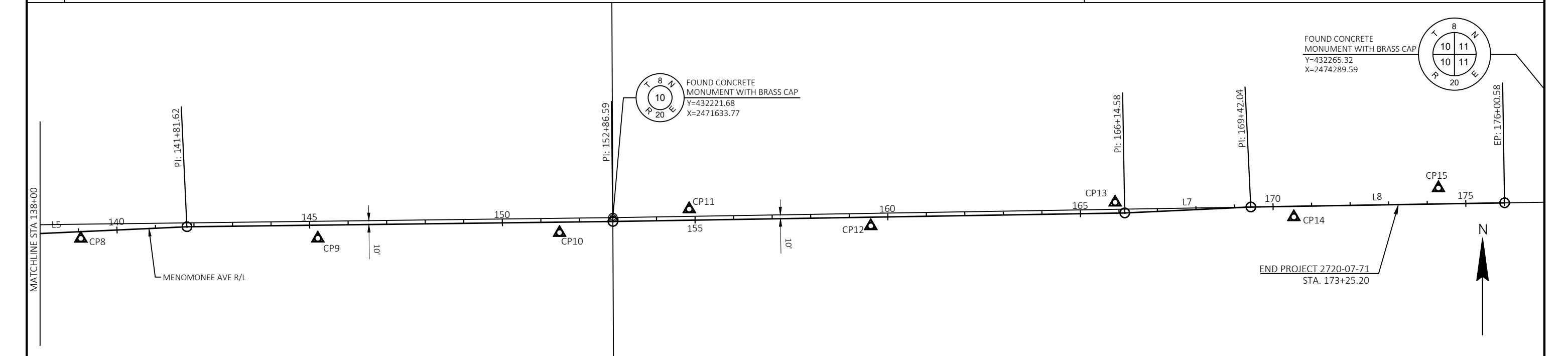
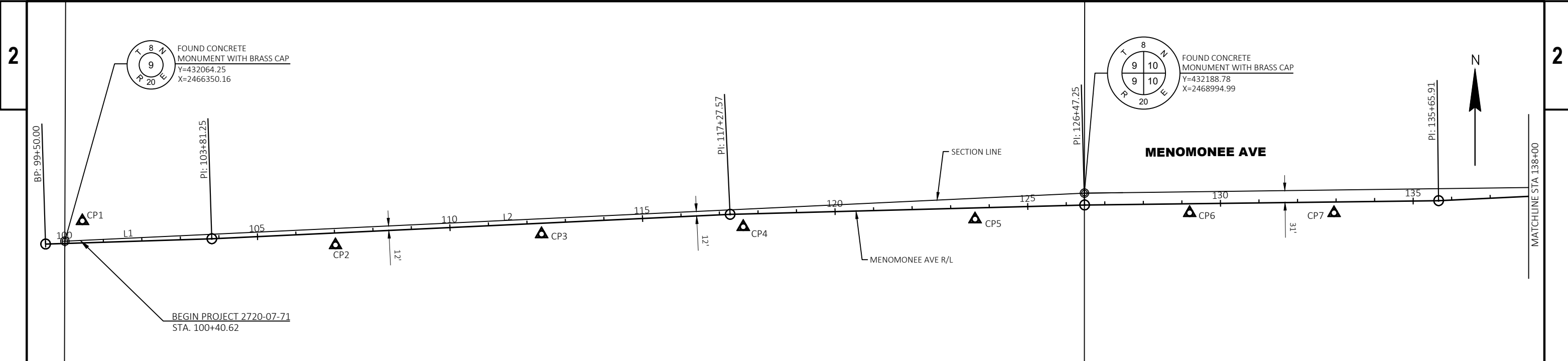


- LEGEND**
- XXXXXXX WORK ZONE
 - ➔ DIRECTION OF TRAFFIC
 - - - - EASTBOUND DETOUR ROUTE
 - ⋯⋯⋯ TRUCK DETOUR ROUTE
 - ⊙ SIGN MOUNTED ON POST
 - PCMS PORTABLE CHANGEABLE MESSAGE SIGN
 - || BARRICADE TYPE III WITH TWO TYPE A WARNING LIGHTS AND ATTACHED SIGN

PCMS MESSAGE SHALL READ:

1ST FRAME	2ND FRAME
EB ROAD TO CLOSE	XXXXDAY XX/XX

- NOTES:**
1. THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.
 2. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS.
 3. THE SPACING BETWEEN TRAFFIC CONTROL AND DETOUR SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.
 4. ALL EXISTING SIGNS THAT CONFLICT WITH TRAFFIC CONTROL SHALL BE COVERED WITH A BLANK ORANGE PANEL AND PAID FOR WITH COVERING SIGNS TYPE II.
 5. MAINTAIN ACCESS WITHIN WORK ZONE TO LOCAL TRAFFIC AND EMERGENCY VEHICLES AT ALL TIMES.
 6. RESTRICT TRUCKS EXCEPT FOR LOCAL DELIVERIES AND CONSTRUCTION VEHICLES IN THE WORK ZONE.
 7. PLACE TRAFFIC CONTROL SIGNS PCMS 7 CALENDAR DAYS PRIOR TO START OF DETOUR.
 8. SEE TRAFFIC CONTROL PLANS FOR ADDITIONAL SIGNS AND TRAFFIC CONTROL DEVICES.
 9. SEE STANDARD DETAIL DRAWINGS FOR LOCAL ROAD AND ADDITIONAL TRAFFIC CONTROL REQUIREMENTS.



MenomoneeAve																
NUMBER	BEGIN LINE (STA)	END LINE (STA)	BEGIN LINE (Y,X)	END LINE (Y,X)	PI (STA)	PI (Y,X)	DELTA	DEGREE	TANGENT	LENGTH	EXTERNAL	RADIUS	PC (STA)	PT (STA)	BEARING BACK	BEARING AHEAD
L1	99+50.00	103+81.25	432056.69 2466300.17	432070.18 2466731.21						431.25'						N88°12'24.4"E
L2	103+81.25	117+27.57	432070.18 2466731.21	432133.50 2468076.04						1346.32'						N87°18'16.0"E
L5	135+65.91	141+81.62	432169.23 2469913.99	432197.90 2470529.03						615.71'						N87°19'51.7"E
L7	166+14.58	169+42.04	432233.50 2472961.73	432248.88 2473288.82						327.46'						N87°18'30.5"E
L8	169+42.04	176+00.58	432248.88 2473288.82	432259.70 2473947.28						658.54'						N89°03'30.5"E

CP	CP TABLE		ELEV	DESCRIPTION
	N	E		
1	432116.4	2466396	847.76	CP IP W/CAP
2	432054.1	2467052	844.35	CP IP W/CAP
3	432082.6	2467587	845.61	CP IP W/CAP
4	432101.3	2468110	844.07	CP IP W/CAP
5	432121	2468711	843.83	CP IP W/CAP
6	432137.4	2469268	844.17	CP IP W/CAP
7	432137.2	2469643	844.29	CP IP W/CAP
8	432167.2	2470254	844.67	CP IP W/CAP
9	432167.7	2470868	849.91	CP IP W/CAP
10	432182.4	2471496	852.39	CP IP W/CAP
11	432243.8	2471832	845.09	CP IP W/CAP
12	432200.1	2472303	819.73	CP IP W/CAP
13	432261.1	2472937	821.16	CP IP W/CAP
14	432223.1	2473401	814CP	IP W/CAP
15	432298.3	2473776	804.06	CP T

Estimate Of Quantities

2720-07-71

Line	Item	Item Description	Unit	Total	Qty
0002	201.0110	Clearing	SY	719.000	719.000
0004	201.0120	Clearing	ID	226.000	226.000
0006	201.0210	Grubbing	SY	719.000	719.000
0008	201.0220	Grubbing	ID	306.000	306.000
0010	203.0100	Removing Small Pipe Culverts	EACH	1.000	1.000
0012	204.0100	Removing Concrete Pavement	SY	1,519.000	1,519.000
0014	204.0130	Removing Curb	LF	329.000	329.000
0016	204.0150	Removing Curb & Gutter	LF	12,102.000	12,102.000
0018	204.0155	Removing Concrete Sidewalk	SY	3,698.000	3,698.000
0020	204.0170	Removing Fence	LF	160.000	160.000
0022	204.0195	Removing Concrete Bases	EACH	2.000	2.000
0024	204.0210	Removing Manholes	EACH	10.000	10.000
0026	204.0215	Removing Catch Basins	EACH	33.000	33.000
0028	204.0245	Removing Storm Sewer (size) 01. Removing Storm Sewer (12-Inch)	LF	696.000	696.000
0030	204.0245	Removing Storm Sewer (size) 02. Removing Storm Sewer (15-Inch)	LF	43.000	43.000
0032	204.0245	Removing Storm Sewer (size) 03. Removing Storm Sewer (18-Inch)	LF	167.000	167.000
0034	204.0245	Removing Storm Sewer (size) 04. Removing Storm Sewer (21-Inch)	LF	192.000	192.000
0036	204.0245	Removing Storm Sewer (size) 05. Removing Storm Sewer (24-Inch)	LF	16.000	16.000
0038	204.0245	Removing Storm Sewer (size) 06. Removing Storm Sewer (30-Inch)	LF	25.000	25.000
0040	204.0245	Removing Storm Sewer (size) 07. Removing Storm Sewer (36-Inch)	LF	516.000	516.000
0042	204.0250	Abandoning Manholes	EACH	9.000	9.000
0044	204.0255	Abandoning Catch Basins	EACH	12.000	12.000
0046	204.0270	Abandoning Culvert Pipes	EACH	27.000	27.000
0048	205.0100	Excavation Common	CY	35,180.000	35,180.000
0050	206.2001	Excavation for Structures Culverts (structure) 01. B-67-401	EACH	1.000	1.000
0052	210.2500	Backfill Structure Type B	TON	1,432.000	1,432.000
0054	213.0100	Finishing Roadway (project) 01. 2720-07-71	EACH	1.000	1.000
0056	305.0110	Base Aggregate Dense 3/4-Inch	TON	6.000	6.000
0058	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	32,356.000	32,356.000
0060	311.0110	Breaker Run	TON	27,356.000	27,356.000
0062	371.2000.S	QMP Base Aggregate Dense 1 1/4-Inch Compaction	EACH	30.000	30.000
0064	416.0610	Drilled Tie Bars	EACH	86.000	86.000
0066	450.4000	HMA Cold Weather Paving	TON	397.000	397.000
0068	455.0605	Tack Coat	GAL	2,357.000	2,357.000
0070	460.2000	Incentive Density HMA Pavement	DOL	6,360.000	6,360.000
0072	460.6223	HMA Pavement 3 MT 58-28 S	TON	5,456.000	5,456.000
0074	460.6224	HMA Pavement 4 MT 58-28 S	TON	4,470.000	4,470.000
0076	465.0120	Asphaltic Surface Driveways and Field Entrances	TON	166.000	166.000
0078	465.0125	Asphaltic Surface Temporary	TON	162.000	162.000
0080	504.0100	Concrete Masonry Culverts	CY	196.000	196.000
0082	505.0400	Bar Steel Reinforcement HS Structures	LB	26,890.000	26,890.000
0084	505.0600	Bar Steel Reinforcement HS Coated Structures	LB	860.000	860.000
0086	511.1200	Temporary Shoring (structure) 01. B-67-401	SF	800.000	800.000
0088	513.2001	Railing Pipe	LF	427.000	427.000
0090	516.0500	Rubberized Membrane Waterproofing	SY	45.000	45.000
0092	520.8000	Concrete Collars for Pipe	EACH	10.000	10.000
0094	522.1015	Apron Endwalls for Culvert Pipe Reinforced Concrete 15-Inch	EACH	1.000	1.000
0096	522.1030	Apron Endwalls for Culvert Pipe Reinforced Concrete 30-Inch	EACH	1.000	1.000
0098	522.1036	Apron Endwalls for Culvert Pipe Reinforced Concrete 36-Inch	EACH	1.000	1.000

Estimate Of Quantities

2720-07-71

Line	Item	Item Description	Unit	Total	Qty
0100	522.2643	Apron Endwalls for Culvert Pipe Reinforced Concrete Horizontal Elliptical 43x68-Inch	EACH	1.000	1.000
0102	601.0105	Concrete Curb Type A	LF	12.000	12.000
0104	601.0110	Concrete Curb Type D	LF	88.000	88.000
0106	601.0407	Concrete Curb & Gutter 18-Inch Type D	LF	86.000	86.000
0108	601.0411	Concrete Curb & Gutter 30-Inch Type D	LF	15,127.000	15,127.000
0110	602.0410	Concrete Sidewalk 5-Inch	SF	62,843.000	62,843.000
0112	602.0515	Curb Ramp Detectable Warning Field Natural Patina	SF	460.000	460.000
0114	602.0810	Concrete Driveway 6-Inch	SY	3,178.000	3,178.000
0116	603.8000	Concrete Barrier Temporary Precast Delivered	LF	814.000	814.000
0118	603.8125	Concrete Barrier Temporary Precast Installed	LF	814.000	814.000
0120	606.0300	Riprap Heavy	CY	123.000	123.000
0122	608.0315	Storm Sewer Pipe Reinforced Concrete Class III 15-Inch	LF	140.000	140.000
0124	608.0330	Storm Sewer Pipe Reinforced Concrete Class III 30-Inch	LF	203.000	203.000
0126	608.0336	Storm Sewer Pipe Reinforced Concrete Class III 36-Inch	LF	511.000	511.000
0128	608.0412	Storm Sewer Pipe Reinforced Concrete Class IV 12-Inch	LF	2,072.000	2,072.000
0130	608.0415	Storm Sewer Pipe Reinforced Concrete Class IV 15-Inch	LF	1,714.000	1,714.000
0132	608.0418	Storm Sewer Pipe Reinforced Concrete Class IV 18-Inch	LF	679.000	679.000
0134	608.0421	Storm Sewer Pipe Reinforced Concrete Class IV 21-Inch	LF	35.000	35.000
0136	608.0424	Storm Sewer Pipe Reinforced Concrete Class IV 24-Inch	LF	954.000	954.000
0138	608.0430	Storm Sewer Pipe Reinforced Concrete Class IV 30-Inch	LF	272.000	272.000
0140	608.2343	Storm Sewer Pipe Reinforced Concrete Horizontal Elliptical Class HE-III 43x68-Inch	LF	598.000	598.000
0142	608.2419	Storm Sewer Pipe Reinforced Concrete Horizontal Elliptical Class HE-IV 19x30-Inch	LF	71.000	71.000
0144	608.2424	Storm Sewer Pipe Reinforced Concrete Horizontal Elliptical Class HE-IV 24x38-Inch	LF	183.000	183.000
0146	608.2434	Storm Sewer Pipe Reinforced Concrete Horizontal Elliptical Class HE-IV 34x53-Inch	LF	318.000	318.000
0148	611.0530	Manhole Covers Type J	EACH	54.000	54.000
0150	611.0545	Manhole Covers Type L	EACH	1.000	1.000
0152	611.0624	Inlet Covers Type H	EACH	102.000	102.000
0154	611.1004	Catch Basins 4-FT Diameter	EACH	35.000	35.000
0156	611.1005	Catch Basins 5-FT Diameter	EACH	1.000	1.000
0158	611.1006	Catch Basins 6-FT Diameter	EACH	1.000	1.000
0160	611.1230	Catch Basins 2x3-FT	EACH	65.000	65.000
0162	611.2003	Manholes 3-FT Diameter	EACH	7.000	7.000
0164	611.2004	Manholes 4-FT Diameter	EACH	17.000	17.000
0166	611.2005	Manholes 5-FT Diameter	EACH	7.000	7.000
0168	611.2006	Manholes 6-FT Diameter	EACH	6.000	6.000
0170	611.2008	Manholes 8-FT Diameter	EACH	6.000	6.000
0172	611.2010	Manholes 10-FT Diameter	EACH	8.000	8.000
0174	611.3003	Inlets 3-FT Diameter	EACH	3.000	3.000
0176	611.8105	Adjusting Catch Basin Covers	EACH	3.000	3.000
0178	611.8110	Adjusting Manhole Covers	EACH	2.000	2.000
0180	611.8115	Adjusting Inlet Covers	EACH	2.000	2.000
0182	612.0406	Pipe Underdrain Wrapped 6-Inch	LF	3,860.000	3,860.000
0184	612.0902.S	Insulation Board Polystyrene (inch) 01. 1-Inch	SY	6.000	6.000
0186	619.1000	Mobilization	EACH	1.000	1.000
0188	620.0200	Concrete Median Blunt Nose	SF	22.000	22.000
0190	620.0300	Concrete Median Sloped Nose	SF	28.000	28.000
0192	624.0100	Water	MGAL	1,271.000	1,271.000
0194	625.0100	Topsoil	SY	13,494.000	13,494.000
0196	625.0500	Salvaged Topsoil	SY	1,430.000	1,430.000

Estimate Of Quantities

2720-07-71

Line	Item	Item Description	Unit	Total	Qty
0198	627.0200	Mulching	SY	15,220.000	15,220.000
0200	628.1504	Silt Fence	LF	2,216.000	2,216.000
0202	628.1905	Mobilizations Erosion Control	EACH	5.000	5.000
0204	628.1910	Mobilizations Emergency Erosion Control	EACH	5.000	5.000
0206	628.2008	Erosion Mat Urban Class I Type B	SY	1,234.000	1,234.000
0208	628.6510	Soil Stabilizer Type B	ACRE	0.300	0.300
0210	628.7005	Inlet Protection Type A	EACH	106.000	106.000
0212	628.7010	Inlet Protection Type B	EACH	2.000	2.000
0214	628.7015	Inlet Protection Type C	EACH	85.000	85.000
0216	628.7020	Inlet Protection Type D	EACH	80.000	80.000
0218	628.7504	Temporary Ditch Checks	LF	16.000	16.000
0220	628.7560	Tracking Pads	EACH	8.000	8.000
0222	628.7570	Rock Bags	EACH	51.000	51.000
0224	629.0210	Fertilizer Type B	CWT	16.000	16.000
0226	630.0140	Seeding Mixture No. 40	LB	76.000	76.000
0228	630.0200	Seeding Temporary	LB	468.000	468.000
0230	630.0500	Seed Water	MGAL	424.500	424.500
0232	631.0300	Sod Water	MGAL	334.500	334.500
0234	631.1000	Sod Lawn	SY	14,843.000	14,843.000
0236	634.0808	Posts Tubular Steel 2x2-Inch X 8-FT	EACH	22.000	22.000
0238	634.0811	Posts Tubular Steel 2x2-Inch X 11-FT	EACH	26.000	26.000
0240	634.0814	Posts Tubular Steel 2x2-Inch X 14-FT	EACH	22.000	22.000
0242	637.2210	Signs Type II Reflective H	SF	329.810	329.810
0244	637.2230	Signs Type II Reflective F	SF	139.000	139.000
0246	638.2602	Removing Signs Type II	EACH	99.000	99.000
0248	638.3000	Removing Small Sign Supports	EACH	54.000	54.000
0250	643.0300	Traffic Control Drums	DAY	73,503.000	73,503.000
0252	643.0410	Traffic Control Barricades Type II	DAY	1,000.000	1,000.000
0254	643.0420	Traffic Control Barricades Type III	DAY	16,064.000	16,064.000
0256	643.0705	Traffic Control Warning Lights Type A	DAY	32,556.000	32,556.000
0258	643.0715	Traffic Control Warning Lights Type C	DAY	1,851.000	1,851.000
0260	643.0900	Traffic Control Signs	DAY	79,198.000	79,198.000
0262	643.1050	Traffic Control Signs PCMS	DAY	14.000	14.000
0264	643.1070	Traffic Control Cones 42-Inch	DAY	2,835.000	2,835.000
0266	643.3105	Temporary Marking Line Paint 4-Inch	LF	7,230.000	7,230.000
0268	643.3150	Temporary Marking Line Removable Tape 4-Inch	LF	8,800.000	8,800.000
0270	643.3165	Temporary Marking Line Paint 6-Inch	LF	356.000	356.000
0272	643.3180	Temporary Marking Line Removable Tape 6-Inch	LF	286.000	286.000
0274	643.3850	Temporary Marking Stop Line Removable Tape 18-Inch	LF	76.000	76.000
0276	643.5000	Traffic Control	EACH	1.000	1.000
0278	644.1430	Temporary Pedestrian Surface Plate	SF	1,390.000	1,390.000
0280	644.1440	Temporary Pedestrian Surface Matting	SF	3,480.000	3,480.000
0282	644.1601	Temporary Pedestrian Curb Ramp	DAY	3,062.000	3,062.000
0284	644.1605	Temporary Pedestrian Detectable Warning Field	SF	340.000	340.000
0286	644.1810	Temporary Pedestrian Barricade	LF	1,545.000	1,545.000
0288	645.0105	Geotextile Type C	SY	292.000	292.000
0290	645.0120	Geotextile Type HR	SY	184.000	184.000
0292	645.0220	Geogrid Type SR	SY	41,088.000	41,088.000
0294	646.1020	Marking Line Epoxy 4-Inch	LF	23,796.000	23,796.000

Estimate Of Quantities

2720-07-71

Line	Item	Item Description	Unit	Total	Qty
0296	646.3020	Marking Line Epoxy 8-Inch	LF	150.000	150.000
0298	646.5020	Marking Arrow Epoxy	EACH	60.000	60.000
0300	646.5120	Marking Word Epoxy	EACH	3.000	3.000
0302	646.5220	Marking Symbol Epoxy	EACH	56.000	56.000
0304	646.6120	Marking Stop Line Epoxy 18-Inch	LF	322.000	322.000
0306	646.6464	Cold Weather Marking Epoxy 4-Inch	LF	4,759.000	4,759.000
0308	646.6468	Cold Weather Marking Epoxy 8-Inch	LF	150.000	150.000
0310	646.7120	Marking Diagonal Epoxy 12-Inch	LF	59.000	59.000
0312	646.7420	Marking Crosswalk Epoxy Transverse Line 6-Inch	LF	1,353.000	1,353.000
0314	646.7520	Marking Crosswalk Epoxy Block Style 24-Inch	LF	202.000	202.000
0316	648.0100	Locating No-Passing Zones	MI	1.380	1.380
0318	650.4000	Construction Staking Storm Sewer	EACH	165.000	165.000
0320	650.4500	Construction Staking Subgrade	LF	7,400.000	7,400.000
0322	650.5000	Construction Staking Base	LF	7,400.000	7,400.000
0324	650.5500	Construction Staking Curb Gutter and Curb & Gutter	LF	15,130.000	15,130.000
0326	650.6501	Construction Staking Structure Layout (structure) 01. B-67-401	EACH	1.000	1.000
0328	650.9000	Construction Staking Curb Ramps	EACH	46.000	46.000
0330	650.9500	Construction Staking Sidewalk (project) 01. 2720-07-71	EACH	1.000	1.000
0332	650.9911	Construction Staking Supplemental Control (project) 01. 2720-07-71	EACH	1.000	1.000
0334	650.9920	Construction Staking Slope Stakes	LF	7,342.000	7,342.000
0336	690.0150	Sawing Asphalt	LF	1,824.000	1,824.000
0338	690.0250	Sawing Concrete	LF	968.000	968.000
0340	715.0502	Incentive Strength Concrete Structures	DOL	1,176.000	1,176.000
0342	740.0440	Incentive IRI Ride	DOL	2,760.000	2,760.000
0344	ASP.1T0A	On-the-Job Training Apprentice at \$5.00/HR	HRS	3,600.000	3,600.000
0346	ASP.1T0G	On-the-Job Training Graduate at \$5.00/HR	HRS	9,600.000	9,600.000
0348	SPV.0060	Special 01. Utility Line Opening	EACH	20.000	20.000
0350	SPV.0060	Special 02. Adjusting Water Valve Box	EACH	72.000	72.000
0352	SPV.0060	Special 03. Adjusting Curb Stop	EACH	22.000	22.000
0354	SPV.0060	Special 04. Adjusting Sanitary Sewer Manhole	EACH	43.000	43.000
0356	SPV.0060	Special 05. Manhole 6' X 3'	EACH	1.000	1.000
0358	SPV.0060	Special 06. Manhole 7.5' X3'	EACH	2.000	2.000
0360	SPV.0060	Special 07. Manhole 9.5' X 7.5'	EACH	1.000	1.000
0362	SPV.0060	Special 08. Inlet Covers Special	EACH	3.000	3.000
0364	SPV.0060	Special 09. Temporary Water Diversion	EACH	4.000	4.000
0366	SPV.0060	Special 10. Remove and Reinstall Mailboxes	EACH	20.000	20.000
0368	SPV.0060	Special 11. Temporary Mailbox Station	EACH	8.000	8.000
0370	SPV.0060	Special 12. Section Corner Monuments	EACH	2.000	2.000
0372	SPV.0060	Special 13. Field Office Type T	EACH	1.000	1.000
0374	SPV.0075	Special 01. Street Sweeping	HRS	50.000	50.000
0376	SPV.0090	Special 01. Asphaltic Curb Temporary	LF	113.000	113.000
0378	SPV.0090	Special 02. Concrete Curb Special	LF	383.000	383.000
0380	SPV.0090	Special 03. Concrete Curb & Gutter 24-Inch Type D	LF	24.000	24.000
0382	SPV.0090	Special 04. Concrete Curb Pedestrian Special	LF	47.000	47.000
0384	SPV.0090	Special 05. Storm Sewer Pipe Reinforced Concrete Horizontal Elliptical Class HE-IV 38X60	LF	564.000	564.000
0386	SPV.0090	Special 06. Storm Sewer Pipe Reinforced Concrete Horizontal Elliptical Class HE-IV 43X68	LF	785.000	785.000
0388	SPV.0090	Special 07. Storm Sewer Pipe Reinforced Concrete Horizontal Elliptical Class HE-IV 58X91	LF	34.000	34.000

Estimate Of Quantities

0390	SPV.0195	Special 01. Box Culvert Fill Material	TON	80.000	2720-07-71 80.000
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CLEARING & GRUBBING

CATEGORY	STATION	TO	STATION	LOCATION	201.0110 CLEARING SY	201.0120 CLEARING ID	201.0210 GRUBBING SY	201.0220 GRUBBING ID
0010	134+63	-	134+63	28.5'LT	-	-	-	24
	135+18	-	135+18	28.2'LT	-	-	-	26
	135+25	-	135+25	27.8'LT	-	-	-	30
	136+22	-	136+22	29.4'LT	-	18	-	18
	136+67	-	136+67	30.9'LT	-	20	-	20
	138+00	-	138+00	33.6'LT	-	24	-	24
	138+33	-	138+33	34.1'LT	-	14	-	14
	143+83	-	143+83	28.5'LT	-	18	-	18
	151+86	-	151+86	28.5'LT	-	30	-	30
	152+39	-	152+39	28.5'LT	-	30	-	30
	153+27	-	153+27	29.8'LT	-	18	-	18
	153+92	-	153+92	28.7'LT	-	18	-	18
	155+06	-	155+06	25.1'LT	-	8	-	8
	156+63	-	156+63	32.9'RT	-	4	-	4
155+00	-	165+00			618	-	618	-
	168+13	-	168+13	36.3'LT	-	24	-	24
170+00	-	174+00			101	-	101	-
TOTAL 0010					719	226	719	306

MISCELLANEOUS REMOVALS

CATEGORY	STATION	TO	STATION	203.0100 REMOVING SMALL PIPE CULVERTS EACH	204.0100 REMOVING CONCRETE PAVEMENT SY	204.0130 REMOVING CURB LF	204.0150 REMOVING CURB & GUTTER LF	204.0155 REMOVING CONCRETE SIDEWALK SY	204.0170 REMOVING FENCE LF	204.0195 REMOVING CONCRETE BASES EACH	SPV.0060.10 REMOVE AND REINSTALL MAILBOXES EACH	SPV.0060.11 TEMPORARY MAILBOX STATION EACH
0010	99+50	-	105+00	1	162	74	433	149	-	-	-	-
	105+00	-	110+00	-	-	-	-	-	-	-	3	1
	110+00	-	115+00	-	13	4	-	-	119	-	2	1
	115+00	-	120+00	-	55	-	643	152	-	-	4	2
	120+00	-	125+00	-	46	-	1,026	259	-	-	3	1
	125+00	-	130+00	-	195	31	1,053	280	-	-	2	1
	130+00	-	135+00	-	117	-	1,040	301	-	-	-	-
	135+00	-	140+00	-	101	140	1,038	186	-	-	-	-
	140+00	-	145+50	-	102	-	1,196	542	-	-	-	-
	145+50	-	150+00	-	131	-	932	520	41	2	-	-
	150+00	-	155+00	-	172	-	1,029	293	-	-	4	1
	155+00	-	160+00	-	133	30	1,065	291	-	-	2	1
	160+00	-	165+00	-	162	-	1,001	273	-	-	-	-
	165+00	-	170+00	-	130	-	997	274	-	-	-	-
	170+00	-	174+00	-	-	50	649	178	-	-	-	-
TOTAL 0010				1	1,519	329	12,102	3,698	160	2	20	8

MISCELLANEOUS REMOVALS

CATEGORY	STATION	TO	STATION	LOCATION	204.0250 ABANDONING MANHOLES EACH	204.0255 ABANDONING CATCH BASIN EACH	204.0270 ABANDONING CULVERT PIPES EACH
0010	105+09,8.6'RT	-	106+93,9.2'RT		-	-	1
			106+71	24.7'LT	-	1	-
			106+93	9.2'RT	1	-	-
106+93,9.2'RT	-	107+41,21.0'RT			-	-	1
			107+41	21.0'RT	-	1	-
			107+42	24.0'LT	-	1	-
			113+68	9.4'RT	1	-	-
113+68,9.4'RT	-	116+62,8.1'RT			-	-	1
			116+62	8.1'RT	1	-	-
116+62,8.1'RT	-	116+89,31.5'RT			-	-	1
			116+93	26.7'LT	-	1	-
116+93,26.7'LT	-	117+13,48.3'LT			-	-	1
			117+16	20.0'RT	-	1	-
117+16,20.0'RT	-	117+18,12.0'LT			-	-	1
117+16,20.0'RT	-	117+19,53.7'RT			-	-	1
117+46,46.8'LT	-	117+63,25.3'LT			-	-	1
			117+63	25.3'LT	-	1	-
120+35,54.1'RT	-	120+36,20.7'RT			-	-	1
120+64,47.7'LT	-	120+81,25.9'LT			-	-	1
			120+81	25.9'LT	-	1	-
			123+11	25.8'LT	-	1	-
123+11,25.8'LT	-	123+30,47.9'LT			-	-	1
123+35,54.9'RT	-	123+35,20.5'RT			-	-	1
123+64,47.2'LT	-	123+81,26.0'LT			-	-	1
			123+81	26.0'LT	-	1	-
126+65,31.9'RT	-	126+65,16.8'RT			-	-	1
			126+65	16.8'RT	-	1	-
126+65,16.8'RT	-	126+66,8.5'LT			-	-	1
134+60,7.3'LT	-	135+80,31.9'RT			-	-	1
			157+76	27.0'RT	1	-	-
157+76,27.0'RT	-	158+02,11.3'RT			-	-	1
			158+02	11.3'RT	1	-	-
158+02,11.3'RT	-	159+42,11.3'RT			-	-	1
159+24,24.4'LT	-	159+32,0.6'RT			-	-	1
			159+25	17.8'LT	-	1	-
159+25,17.8'LT	-	159+32,0.6'RT			-	-	1
			159+32	0.6'RT	1	-	-
159+32,0.6'RT	-	159+88,0.5'LT			-	-	1
			159+42	11.3'RT	1	-	-
159+42,11.3'RT	-	163+89,0.3'RT			-	-	1
			159+83	17.7'RT	-	1	-
159+83,17.7'RT	-	159+88,0.5'LT			-	-	1
159+83,24.4'LT	-	159+88,0.5'LT			-	-	1
			159+88	0.5'LT	1	-	-
159+88,0.5'LT	-	163+89,0.3'RT			-	-	1
163+75,17.7'RT	-	163+89,0.3'RT			-	-	1
			163+89	0.3'RT	1	-	-
163+89,0.3'RT	-	164+00,29.5'LT			-	-	1
TOTAL 0010					9	12	27

DRAINAGE REMOVALS

CATEGORY	STATION	TO	STATION	LOCATION	204.0210	204.0215	204.0245	204.0245	204.0245	204.0245	204.0245	204.0245	204.0245
					REMOVING MANHOLES EACH	REMOVING CATCH BASINS EACH	REMOVING STORM SEWER 12-INCH LF	REMOVING STORM SEWER 15-INCH LF	REMOVING STORM SEWER 18-INCH LF	REMOVING STORM SEWER 21-INCH LF	REMOVING STORM SEWER 24-INCH LF	REMOVING STORM SEWER 30-INCH LF	REMOVING STORM SEWER 36-INCH LF
0010			100+21	7.1'RT	1	-	-	-	-	-	-	-	-
	100+21, 7.1'RT	-	102+48, 10.2'RT										227
			102+48	10.2'RT	1	-	-	-	-	-	-	-	-
	102+48, 10.2'RT	-	102+48, 18.9'RT				9	-	-	-	-	-	-
			102+48	18.9'RT	-	1	-	-	-	-	-	-	-
	102+48, 10.2'RT	-	102+48, 27.5'LT				38	-	-	-	-	-	-
			102+48	27.5'LT	-	1	-	-	-	-	-	-	-
	102+48, 10.2'RT	-	104+48, 50.7'RT				-	-	-	-	-	-	219
			103+65	40.6'RT	-	1	-	-	-	-	-	-	-
	103+65, 40.6'RT	-	103+98, 40.6'RT				33	-	-	-	-	-	-
			103+98	40.6'RT	-	1	-	-	-	-	-	-	-
	103+98, 40.6'RT	-	104+40, 59.9'RT				46	-	-	-	-	-	-
	104+63, 52.3'RT	-	105+09, 8.6'RT				-	-	70	-	-	-	-
	106+71, 24.7'LT	-	106+89, 47.7'LT				29	-	-	-	-	-	-
			106+89	47.7'LT	-	1	-	-	-	-	-	-	-
	106+89, 47.7'LT	-	106+96, 46.5'LT				7	-	-	-	-	-	-
	106+93, 9.2'RT	-	106+96, 46.5'LT				-	-	55	-	-	-	-
			106+96	46.5'LT	1	-	-	-	-	-	-	-	-
	106+96, 46.5'LT	-	106+97, 64.5'LT				18	-	-	-	-	-	-
	106+96, 46.5'LT	-	107+21, 48.8'LT				26	-	-	-	-	-	-
			107+21	48.8'LT	-	1	-	-	-	-	-	-	-
	107+21, 48.8'LT	-	107+42, 24.0'LT				32	-	-	-	-	-	-
			113+62	45.9'LT	-	1	-	-	-	-	-	-	-
	113+62, 45.9'LT	-	113+69, 43.6'LT				8	-	-	-	-	-	-
	113+68, 9.4'RT	-	113+69, 43.6'LT				-	-	53	-	-	-	-
			113+69	43.6'LT	1	-	-	-	-	-	-	-	-
	113+69, 43.6'LT	-	113+70, 48.1'LT				-	-	5	-	-	-	-
	113+69, 43.6'LT	-	113+96, 47.1'LT				27	-	-	-	-	-	-
			113+96	47.1'LT	-	1	-	-	-	-	-	-	-
			117+13	48.3'LT	-	1	-	-	-	-	-	-	-
	117+13, 48.3'LT	-	117+20, 48.0'LT				7	-	-	-	-	-	-
	117+18, 12.0'LT	-	117+20, 48.0'LT				-	-	37	-	-	-	-
			117+20	48.0'LT	1	-	-	-	-	-	-	-	-
	117+20, 48.0'LT	-	117+46, 46.8'LT				28	-	-	-	-	-	-
			117+46	46.8'LT	-	1	-	-	-	-	-	-	-
			120+12	25.8'LT	-	1	-	-	-	-	-	-	-
	120+12, 25.8'LT	-	120+30, 48.3'LT				29	-	-	-	-	-	-
			120+30	48.3'LT	-	1	-	-	-	-	-	-	-
	120+30, 48.3'LT	-	120+37, 48.9'LT				7	-	-	-	-	-	-
	120+36, 20.7'RT	-	120+37, 48.9'LT				-	-	70	-	-	-	-
			120+36	20.7'RT	-	1	-	-	-	-	-	-	-
			120+37	48.9'LT	1	-	-	-	-	-	-	-	-
	120+37, 48.9'LT	-	120+64, 47.7'LT				28	-	-	-	-	-	-
			120+64	47.7'LT	-	1	-	-	-	-	-	-	-
			123+30	47.9'LT	-	1	-	-	-	-	-	-	-
	123+30, 47.9'LT	-	123+37, 47.7'LT				7	-	-	-	-	-	-
	123+35, 20.5'RT	-	123+37, 47.7'LT				-	-	69	-	-	-	-
			123+35, 20.5'RT	20.5'RT	-	1	-	-	-	-	-	-	-
			123+37	47.7'LT	1	-	-	-	-	-	-	-	-
	123+37, 47.7'LT	-	123+64, 47.2'LT				27	-	-	-	-	-	-
			SUBTOTAL		7	16	406	-	167	192	-	-	446

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DRAINAGE REMOVALS

CATEGORY	STATION	TO	STATION	LOCATION	204.0210	204.0215	204.0245	204.0245	204.0245	204.0245	204.0245	204.0245	204.0245
					REMOVING MANHOLES EACH	REMOVING CATCH BASINS EACH	REMOVING STORM SEWER 12-INCH LF	REMOVING STORM SEWER 15-INCH LF	REMOVING STORM SEWER 18-INCH LF	REMOVING STORM SEWER 21-INCH LF	REMOVING STORM SEWER 24-INCH LF	REMOVING STORM SEWER 30-INCH LF	REMOVING STORM SEWER 36-INCH LF
0010			123+64	47.2'LT	-	1	-	-	-	-	-	-	-
			124+92	52.5'LT	-	1	-	-	-	-	-	-	-
			125+99	25.8'LT	-	1	-	-	-	-	-	-	-
		125+99,25.8'LT -	126+66,8.5'LT			-	-	70	-	-	-	-	-
			126+66	8.5'LT	1	-	-	-	-	-	-	-	-
		126+66,8.5'LT -	126+66,39.4'LT			-	-	31	-	-	-	-	-
		126+66,8.5'LT -	126+81,29.7'LT			-	-	26	-	-	-	-	-
			126+81	29.7'LT	-	1	-	-	-	-	-	-	-
			134+11	24.5'LT	-	1	-	-	-	-	-	-	-
		134+11,24.5'LT -	134+60,7.3'LT			-	-	51	-	-	-	-	-
			135+81	17.5'RT	-	1	-	-	-	-	-	-	-
			157+34	17.4'RT	-	1	-	-	-	-	-	-	-
		157+34,17.4'RT -	157+54,40.9'RT			-	-	31	-	-	-	-	-
			157+54	40.9'RT	-	1	-	-	-	-	-	-	-
		157+54,40.9'RT -	157+57,50.6'RT			-	-	16	-	-	-	-	-
		157+57,50.6'RT -	157+76,27.0'RT			-	-	-	-	-	-	25	-
		157+76,27.0'RT -	157+88,40.6'RT			-	-	19	-	-	-	-	-
			157+88	40.6'RT	-	1	-	-	-	-	-	-	-
			159+24	24.4'LT	-	1	-	-	-	-	-	-	-
		159+24,24.4'LT -	159+38,39.7'LT			-	-	21	-	-	-	-	-
			159+38	39.7'LT	-	1	-	-	-	-	-	-	-
			159+67	42.6'LT	-	1	-	-	-	-	-	-	-
		159+67,42.6'LT -	159+83,24.4'LT			-	-	25	-	-	-	-	-
			159+83	24.4'LT	-	1	-	-	-	-	-	-	-
		163+63,28.7'RT -	163+75,18.4'RT			-	-	-	-	-	16	-	-
		163+72,9.9'RT -	163+89,0.3'RT			-	-	-	-	-	-	-	19
			163+75	17.7'RT	-	1	-	-	-	-	-	-	-
			164+00	29.5'LT	1	-	-	-	-	-	-	-	-
			164+00	24.9'LT	-	1	-	-	-	-	-	-	-
		164+00,29.5'LT -	164+31,69.9'LT			-	-	-	-	-	-	-	51
		172+81	19.7'LT	-	1	-	-	-	-	-	-	-	
	172+81,19.7'LT -	172+82,8.6'RT			-	-	-	28	-	-	-	-	
		172+81	19.9'RT	-	1	-	-	-	-	-	-	-	
	172+81,19.9'RT -	172+82,8.6'RT			-	-	-	11	-	-	-	-	
		172+82	8.6'RT	1	-	-	-	-	-	-	-	-	
	172+82,8.6'RT -	173+17,10.5'LT			-	-	-	4	-	-	-	-	
		SUBTOTAL			3	17	290	43	-	-	16	25	70
		TOTAL 0010			10	33	696	43	167	192	16	25	516

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3

EARTHWORK SUMMARY		A	B	C	D	#	F	G	H	I
		ITEM #205.0100		*	*	ITEM #311.0110	*	*	*	*
LOCATION	FROM/TO STATION	COMMON EXCAVATION (1) (CY)		SALVAGED/UNUSABLE PAVEMENT MATERIAL (6) (CY)	AVAILABLE MATERIAL (7) (CY)	EBS BACKFILL = BREAKER RUN (3) (TON)	UNEXPANDED FILL (CY)	EXPANDED FILL (8) (CY)	MASS ORDINATE +/- (9) (CY)	WASTE (CY)
		CUT (2) (CY)	EBS EXCAVATION(3)(4)(5) (CY)			CONVERSION FACTOR 1.90		FACTOR 1.25		
MENOMONEE AVENUE	100+30 - 174+00	34,060	0	4,245	29,815	0	235	294	29,521	29,521
UNDISTRIBUTED (EBS)		420	700	0	0	1,330	0	0	0	0
GRAND TOTAL CAT 0010		35,180		4,245	29,815	1,330	235	294	29,521	29,521

NOTES:

*NOT A BID ITEM. COLUMN SHOWN FOR INFORMATION ONLY.

ADDITIONAL QUANTITIES SHOWN ELSEWHERE. SEE "AGGREGATE" TABLE.

1) COMMON EXCAVATION IS THE SUM OF THE CUT (A) AND EXCAVATION BELOW SUBGRADE (B) COLUMNS. ITEM NUMBER 205.0100.

2) SALVAGED/UNUSABLE PAVEMENT MATERIAL (C) IS INCLUDED IN THE CUT (A) TO THE PROPOSED SUBGRADE LINE.

3) EBS EXCAVATION (B) TO BE BACKFILLED WITH BREAKER RUN (E) WITH GEOGRID TYPE SR. BREAKER RUN UNIT WEIGHT ESTIMATED AT 1.9 TONS PER CY. ADDITIONAL QUANTITIES SHOWN ELSEWHERE, SEE 'BASE AGGREGATE ITEMS' TABLE.

4) EBS AREAS WERE APPROXIMATED AS SPECIFIED IN THE GEOTECHNICAL REPORT AND ARE NOT SHOWN ON CROSS SECTIONS. ACTUAL EBS LOCATIONS TO BE DETERMINED BY ENGINEER.

5) EBS EXCAVATION MATERIAL SHALL BE REMOVED FROM THE SITE AND SHALL NOT BE USED AS FILL MATERIAL. EBS EXCAVATION MATERIAL IS NOT INCLUDED IN THE MASS ORDINATE.

6) SALVAGED/UNUSABLE PAVEMENT MATERIAL (C) IS INCLUDED IN THE COMMON EXCAVATION QUANTITY TO THE BOTTOM OF THE EXISTING PAVEMENT. THIS ASSUMES THE EXISTING PAVEMENT STRUCTURE IS SALVAGED OR WASTED BY THE CONTRACTOR.

THE EXISTING PAVEMENT STRUCTURE IS NOT SHOWN ON THE CROSS SECTIONS.

7) AVAILABLE MATERIAL (D) = CUT (A) - SALVAGED/UNUSABLE PAVEMENT MATERIAL (C).

8) EXPANDED FILL (G) = UNEXPANDED FILL (F) * EXPANDED FILL FACTOR (1.25).

9) THE MASS ORDINATE (H) + OR - QTY CALCULATED FOR THE DIVISION. PLUS QUANTITY INDICATES AN EXCESS OF MATERIAL WITHIN THE DIVISION. MINUS INDICATES A SHORTAGE OF MATERIAL WITHIN THE DIVISION.

3

3

AGGREGATE

CATEGORY	STATION TO STATION	LOCATION	305.0110	305.0120	311.0110	624.0100	645.0220
			BASE AGGREGATE DENSE 3/4-INCH TON	BASE AGGREGATE DENSE 1 1/4-INCH TON	BREAKER RUN TON	WATER MGAL	GEOGRID TYPE SR SY
0010	99+50 - 105+00		3	2,341	1,862	89	2,940
	105+00 - 110+00		3	2,215	1,799	86	2,840
	110+00 - 115+00		-	2,218	1,803	86	2,846
	115+00 - 120+00		-	2,174	1,756	83	2,772
	120+00 - 125+00		-	2,236	1,854	88	2,927
	125+00 - 130+00		-	2,342	1,896	90	2,993
	130+00 - 135+00		-	2,249	1,819	87	2,872
	135+00 - 140+00		-	2,390	1,846	90	2,914
	140+00 - 145+50		-	2,460	1,989	95	3,140
	145+50 - 150+00		-	2,035	1,648	78	2,602
	150+00 - 155+00		-	2,247	1,816	87	2,867
	155+00 - 160+00		-	2,278	1,853	87	2,925
	160+00 - 165+00		-	2,086	1,654	80	2,612
	165+00 - 170+00		-	1,860	1,451	70	2,290
	170+00 - 174+00		-	1,225	980	47	1,548
UNDISTRIBUTED					1,330	28	
TOTAL 0010			6	32,356	27,356	1,271	41,088

ASPHALT

CATEGORY	STATION TO STATION	LOCATION	450.4000	455.0605	460.6223	460.6224	465.0120	465.0125	SPV.0090.01
			HMA COLD WEATHER PAVING TON	TACK COAT GAL	HMA Pavement 3 MT 58-28 S TON	HMA Pavement 4 MT 58-28 S TON	ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES TON	ASPHALTIC SURFACE TEMPORARY TON	ASPHALTIC CURB TEMPORARY LF
0010	99+50 - 105+00		-	172	398	326	14	80	58
	105+00 - 110+00		-	163	377	309	21	24	-
	110+00 - 115+00		-	164	380	311	18	-	-
	115+00 - 120+00		-	158	367	300	18	-	-
	120+00 - 125+00		-	169	392	321	-	-	-
	125+00 - 130+00		-	173	400	328	3	-	-
	130+00 - 135+00		-	165	382	313	17	-	-
	135+00 - 140+00		-	168	388	318	16	-	-
	140+00 - 145+50		-	180	417	342	8	9	55
	145+50 - 150+00		-	150	346	283	-	-	-
	150+00 - 155+00		-	165	382	313	1	-	-
	155+00 - 160+00		-	173	400	328	13	-	-
	160+00 - 165+00		-	148	343	281	-	-	-
	165+00 - 170+00		-	125	290	238	22	-	-
	170+00 - 174+00		-	84	194	159	15	-	-
UNDISTRIBUTED				397				49	
TOTAL 0010			397	2,357	5,456	4,470	166	162	113

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CONCRETE

CATEGORY	STATION TO STATION	LOCATION	416.0610	601.0105	601.0110	601.0407	601.0411	602.0410	602.0515	602.0810	620.0200	620.0300	SPV.0090.02	SPV.0090.03	SPV.0090.04
			DRILLED TIE BARS EACH	CONCRETE CURB TYPE A LF	CONCRETE CURB TYPE D LF	CONCRETE CURB & GUTTER 18-INCH TYPE D LF	CONCRETE CURB & GUTTER 30-INCH TYPE D LF	CONCRETE SIDEWALK 5-INCH SF	CONCRETE WARNING FIELD NATURAL PATINA SF	CONCRETE DRIVEWAY 6-INCH SY	CONCRETE MEDIAN BLUNT NOSE SF	CONCRETE MEDIAN SLOPED NOSE SF	CONCRETE CURB SPECIAL SF	CONCRETE CURB SPECIAL LF	CONCRETE CURB PEDESTRIAN SPECIAL LF
0010	99+50 - 105+00		12	12	-	-	997	1,692	20	247	22	-	-	24	-
	105+00 - 110+00		0	-	-	-	1,024	4,391	20	143	-	-	-	-	-
	110+00 - 115+00		0	-	-	-	1,045	4,253	20	191	-	-	-	-	-
	115+00 - 120+00		8	-	-	37	1,030	4,399	40	199	-	-	-	-	-
	120+00 - 125+00		8	-	-	-	1,029	4,537	40	64	-	-	-	-	-
	125+00 - 130+00		8	-	-	49	1,018	4,437	40	229	-	28	-	-	-
	130+00 - 135+00		4	-	-	-	1,038	4,502	20	204	-	-	-	-	-
	135+00 - 140+00		8	-	36	-	1,049	4,419	40	497	-	-	-	-	-
	140+00 - 145+50		8	-	28	-	1,169	5,028	60	212	-	-	-	23	-
	145+50 - 150+00		8	-	-	-	932	4,319	80	164	-	-	-	-	-
	150+00 - 155+00		8	-	-	-	1,028	4,210	40	233	-	-	-	-	-
	155+00 - 160+00		8	-	-	-	1,060	4,191	40	215	-	-	-	-	-
	160+00 - 165+00		0	-	-	-	1,001	5,018	0	229	-	-	383	-	-
	165+00 - 170+00		4	-	-	-	1,001	4,232	0	278	-	-	-	-	24
	170+00 - 174+00		2	-	24	-	706	3,215	0	73	-	-	-	-	-
TOTAL 0010			86	12	88	86	15,127	62,843	460	3,178	22	28	383	47	24

STORM SEWER

		608.0315	608.0330	608.0336	608.0412	608.0415	608.0418	608.0421	608.0424	608.0430	608.2343	608.2419	608.2424	608.2434	SPV.0090.05	SPV.0090.06	SPV.0090.07				
		STORM SEWER PIPE REINFORCED CONCRETE										STORM SEWER PIPE REINFORCED CONCRETE HORIZONTAL ELLIPTICAL									
		CLASS III	CLASS III	CLASS III	CLASS IV	CLASS IV	CLASS IV	CLASS IV	CLASS IV	CLASS IV	CLASS HE-III	CLASS HE-IV	CLASS HE-IV	CLASS HE-IV	CLASS HE-IV	CLASS HE-IV	CLASS HE-IV	CLASS HE-IV	JOINTTIES*		
FROM - TO	INLET ELEVATION	DISCHARGE ELEVATION	SLOPE FT/FT	15-INCH LF	30-INCH LF	36-INCH LF	12-INCH LF	15-INCH LF	18-INCH LF	21-INCH LF	24-INCH LF	30-INCH LF	43X68-INCH LF	19X30-INCH LF	24X38-INCH LF	34X53-INCH LF	38X60-INCH LF	43X68-INCH LF	58X91-INCH LF	EACH	
1 - 10	841.61	840.48	0.0049	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	229	-	-
10 - 11	840.48	840.10	0.0050	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	75	-	-
10A - 10	842.20	842.03	0.0049	-	-	-	35	-	-	-	-	-	-	-	-	-	-	-	-	-	-
10B - 10	843.12	843.06	0.0050	-	-	-	12	-	-	-	-	-	-	-	-	-	-	-	-	-	-
11 - 12D	840.10	839.70	0.0050	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	81	-	-
11A - 11	842.86	842.69	0.0050	-	-	-	34	-	-	-	-	-	-	-	-	-	-	-	-	-	-
11B - 11	842.75	842.69	0.0050	-	-	-	13	-	-	-	-	-	-	-	-	-	-	-	-	-	-
12A - 12B	841.39	841.29	0.0068	-	-	-	-	15	-	-	-	-	-	-	-	-	-	-	-	-	-
12B - 12C	841.29	840.73	0.0161	-	-	-	-	35	-	-	-	-	-	-	-	-	-	-	-	-	-
12C - 12D	840.48	840.33	0.0103	-	-	-	-	-	15	-	-	-	-	-	-	-	-	-	-	-	-
12D - 12W	839.70	839.50	0.0078	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	26	-	6
21 - 19W	839.69	839.50	0.0024	-	-	-	-	-	-	-	79	-	-	-	-	-	-	-	-	-	6
22 - 21	839.94	839.69	0.0014	-	-	-	-	-	-	-	-	-	-	179	-	-	-	-	-	-	-
23 - 22	840.11	839.94	0.0045	-	-	-	-	-	37	-	-	-	-	-	-	-	-	-	-	-	-
23A - 23B	840.68	840.62	0.0016	-	-	-	37	-	-	-	-	-	-	-	-	-	-	-	-	-	-
23B - 23	840.62	840.54	0.0095	-	-	-	8	-	-	-	-	-	-	-	-	-	-	-	-	-	-
23C - 23	840.71	840.58	0.0048	-	-	-	27	-	-	-	-	-	-	-	-	-	-	-	-	-	-
23D - 23	840.31	840.11	0.0246	-	-	-	8	-	-	-	-	-	-	-	-	-	-	-	-	-	-
24 - 22	840.06	839.94	0.0017	-	-	-	-	-	-	-	-	-	-	71	-	-	-	-	-	-	-
24A - 24B	840.49	840.42	0.0050	-	-	-	-	15	-	-	-	-	-	-	-	-	-	-	-	-	-
24B - 24	840.42	840.38	0.0057	-	-	-	-	7	-	-	-	-	-	-	-	-	-	-	-	-	-
24C - 24D	840.80	840.77	0.0030	-	-	-	10	-	-	-	-	-	-	-	-	-	-	-	-	-	-
24D - 24	840.77	840.60	0.0050	-	-	-	34	-	-	-	-	-	-	-	-	-	-	-	-	-	-
24W - 24D	841.50	840.77	0.0523	-	-	-	17	-	-	-	-	-	-	-	-	-	-	-	-	-	-
25 - 24	840.84	840.06	0.0025	-	-	-	-	-	310	-	-	-	-	-	-	-	-	-	-	-	-
25A - 25	841.41	841.34	0.0100	-	-	-	7	-	-	-	-	-	-	-	-	-	-	-	-	-	-
25B - 25	841.68	841.34	0.0100	-	-	-	34	-	-	-	-	-	-	-	-	-	-	-	-	-	-
26 - 25	841.40	841.09	0.0025	-	-	-	-	125	-	-	-	-	-	-	-	-	-	-	-	-	-
26W - 26	844.10	841.65	0.0354	-	-	-	79	-	-	-	-	-	-	-	-	-	-	-	-	-	-
30 - 31	841.70	841.45	0.0089	-	-	-	-	-	-	-	28	-	-	-	-	-	-	-	-	-	-
30A - 30	842.79	842.70	0.0108	-	-	-	8	-	-	-	-	-	-	-	-	-	-	-	-	-	-
30B - 30	842.98	842.70	0.0101	-	-	-	28	-	-	-	-	-	-	-	-	-	-	-	-	-	-
30C - 30	841.99	841.85	0.0342	-	-	-	-	-	4	-	-	-	-	-	-	-	-	-	-	-	-
31 - 32	841.45	839.51	0.0076	-	-	-	-	-	-	-	256	-	-	-	-	-	-	-	-	-	-
32 - 39	839.01	838.67	0.0036	-	-	-	-	-	-	-	-	94	-	-	-	-	-	-	-	-	-
32A - 32	840.25	840.18	0.0100	-	-	-	7	-	-	-	-	-	-	-	-	-	-	-	-	-	-
32B - 32	840.52	840.18	0.0100	-	-	-	34	-	-	-	-	-	-	-	-	-	-	-	-	-	-
39 - 41	838.67	838.49	0.0022	-	-	-	-	-	-	-	-	-	-	-	82	-	-	-	-	-	-
40 - 39	839.12	838.89	0.0076	-	-	-	-	-	-	-	31	-	-	-	-	-	-	-	-	-	-
40A - 40B	839.88	839.77	0.0050	-	-	-	22	-	-	-	-	-	-	-	-	-	-	-	-	-	-
40B - 40	839.77	839.62	0.0205	-	-	-	7	-	-	-	-	-	-	-	-	-	-	-	-	-	-
40C - 40	839.76	839.62	0.0050	-	-	-	28	-	-	-	-	-	-	-	-	-	-	-	-	-	-
40D - 40C	839.85	839.76	0.0049	-	-	-	18	-	-	-	-	-	-	-	-	-	-	-	-	-	-
40E - 40	839.75	839.68	0.0179	-	-	-	-	-	4	-	-	-	-	-	-	-	-	-	-	-	-
41 - 45	838.49	838.02	0.0020	-	-	-	-	-	-	-	-	-	-	-	-	236	-	-	-	-	-
SUBTOTAL				-	-	-	507	197	370	-	315	173	-	71	179	318	-	411	-	12	

*NON-BID ITEM: FOR INFORMATION ONLY

STORM SEWER

		608.0315	608.0330	608.0336	608.0412	608.0415	608.0418	608.0421	608.0424	608.0430	608.2343	608.2419	608.2424	608.2434	SPV.0090.05	SPV.0090.06	SPV.0090.07			
		STORM SEWER PIPE REINFORCED CONCRETE										STORM SEWER PIPE REINFORCED CONCRETE HORIZONTAL ELLIPTICAL								
		CLASS III	CLASS III	CLASS III	CLASS IV	CLASS IV	CLASS IV	CLASS IV	CLASS IV	CLASS IV	CLASS HE-III	CLASS HE-IV	CLASS HE-IV	CLASS HE-IV	CLASS HE-IV	CLASS HE-IV	CLASS HE-IV	CLASS HE-IV	JOINTTIES*	
FROM - TO	INLET ELEVATION	DISCHARGE ELEVATION	SLOPE FT/FT	15-INCH LF	30-INCH LF	36-INCH LF	12-INCH LF	15-INCH LF	18-INCH LF	21-INCH LF	24-INCH LF	30-INCH LF	43X68-INCH LF	19X30-INCH LF	24X38-INCH LF	34X53-INCH LF	38X60-INCH LF	43X68-INCH LF	58X91-INCH LF	EACH
41B - 41	839.76	839.67	0.0150	-	-	-	6	-	-	-	-	-	-	-	-	-	-	-	-	-
41C - 41	839.88	839.67	0.0051	-	-	-	42	-	-	-	-	-	-	-	-	-	-	-	-	-
41D - 41C	839.92	839.87	0.0050	-	-	-	10	-	-	-	-	-	-	-	-	-	-	-	-	-
44 - 45	838.34	838.04	0.0096	-	-	-	-	-	-	-	31	-	-	-	-	-	-	-	-	-
44A - 44B	840.04	839.88	0.0050	-	-	-	33	-	-	-	-	-	-	-	-	-	-	-	-	-
44B - 44	839.63	839.59	0.0055	-	-	-	-	7	-	-	-	-	-	-	-	-	-	-	-	-
44C - 44	839.98	839.84	0.0050	-	-	-	28	-	-	-	-	-	-	-	-	-	-	-	-	-
44D - 44	839.11	839.09	0.0038	-	-	-	4	-	-	-	-	-	-	-	-	-	-	-	-	-
45 - 50	838.02	837.68	0.0020	-	-	-	-	-	-	-	-	-	-	-	-	-	174	-	-	-
45A - 45	839.86	839.52	0.0097	-	-	-	35	-	-	-	-	-	-	-	-	-	-	-	-	-
50 - 55	837.68	837.40	0.0022	-	-	-	-	-	-	-	-	-	-	-	-	-	126	-	-	-
50A - 50B	839.42	839.32	0.0100	-	-	-	10	-	-	-	-	-	-	-	-	-	-	-	-	-
50B - 50	839.32	839.25	0.0100	-	-	-	7	-	-	-	-	-	-	-	-	-	-	-	-	-
50C - 50	839.42	839.25	0.0050	-	-	-	34	-	-	-	-	-	-	-	-	-	-	-	-	-
50D - 50C	839.47	839.42	0.0050	-	-	-	10	-	-	-	-	-	-	-	-	-	-	-	-	-
55 - 57	837.40	837.17	0.0015	-	-	-	-	-	-	-	-	-	-	-	-	-	156	-	-	-
55A - 55	839.91	839.57	0.0100	-	-	-	34	-	-	-	-	-	-	-	-	-	-	-	-	-
56 - 55	837.84	837.71	0.0042	-	-	-	-	-	-	31	-	-	-	-	-	-	-	-	-	-
56A - 56	838.46	838.30	0.0205	-	-	-	8	-	-	-	-	-	-	-	-	-	-	-	-	-
56B - 56	838.86	838.30	0.0204	-	-	-	28	-	-	-	-	-	-	-	-	-	-	-	-	-
56C - 56	837.92	837.91	0.0015	-	-	-	-	-	-	4	-	-	-	-	-	-	-	-	-	-
57 - 60	837.17	837.01	0.0015	-	-	-	-	-	-	-	-	-	-	-	-	-	108	-	-	-
57A - 57	838.31	838.04	0.0075	-	-	-	36	-	-	-	-	-	-	-	-	-	-	-	-	-
60 - 61	837.01	836.91	0.0015	-	-	-	-	-	-	-	-	-	-	-	-	-	-	66	-	-
60A - 60B	839.10	838.96	0.0075	-	-	-	18	-	-	-	-	-	-	-	-	-	-	-	-	-
60B - 60	838.96	838.91	0.0075	-	-	-	7	-	-	-	-	-	-	-	-	-	-	-	-	-
60C - 60	839.17	838.91	0.0075	-	-	-	34	-	-	-	-	-	-	-	-	-	-	-	-	-
60D - 60C	839.32	839.17	0.0075	-	-	-	20	-	-	-	-	-	-	-	-	-	-	-	-	-
60E - 60A	839.24	839.10	0.0075	-	-	-	18	-	-	-	-	-	-	-	-	-	-	-	-	-
61 - 62	836.91	836.87	0.0021	-	-	-	-	-	-	-	-	-	-	-	-	-	-	18	-	-
61A - 61	837.65	837.58	0.0030	-	-	-	-	-	-	23	-	-	-	-	-	-	-	-	-	-
62 - 64	836.87	836.38	0.0017	-	-	-	-	-	-	-	-	-	-	-	-	-	-	290	-	-
62A - 62	839.57	839.45	0.0100	-	-	-	12	-	-	-	-	-	-	-	-	-	-	-	-	-
63 - 64	840.53	840.29	0.0100	-	-	-	23	-	-	-	-	-	-	-	-	-	-	-	-	-
63A - 63B	840.92	840.75	0.0100	-	-	-	17	-	-	-	-	-	-	-	-	-	-	-	-	-
63B - 63	840.75	840.53	0.0100	-	-	-	22	-	-	-	-	-	-	-	-	-	-	-	-	-
63C - 63D	840.81	840.63	0.0100	-	-	-	18	-	-	-	-	-	-	-	-	-	-	-	-	-
63D - 63	840.63	840.53	0.0100	-	-	-	10	-	-	-	-	-	-	-	-	-	-	-	-	-
64 - 70	836.38	835.57	0.0030	-	-	-	-	-	-	-	-	-	273	-	-	-	-	-	-	-
70 - 71	835.57	835.07	0.0030	-	-	-	-	-	-	-	-	169	-	-	-	-	-	-	-	-
70A - 70B	839.53	839.36	0.0050	-	-	-	34	-	-	-	-	-	-	-	-	-	-	-	-	-
70B - 70C	839.36	839.17	0.0099	-	-	-	19	-	-	-	-	-	-	-	-	-	-	-	-	-
70C - 70	839.17	839.04	0.0104	-	-	-	12	-	-	-	-	-	-	-	-	-	-	-	-	-
70D - 70	839.38	839.04	0.0100	-	-	-	34	-	-	-	-	-	-	-	-	-	-	-	-	-
71 - 72	835.07	834.61	0.0029	-	-	-	-	-	-	-	-	156	-	-	-	-	-	-	-	-
SUBTOTAL				-	-	-	623	7	-	35	23	31	598	-	-	-	564	374	-	-

*NON-BID ITEM: FOR INFORMATION ONLY

STORM SEWER

		608.0315	608.0330	608.0336	608.0412	608.0415	608.0418	608.0421	608.0424	608.0430	608.2343	608.2419	608.2424	608.2434	SPV.0090.05	SPV.0090.06	SPV.0090.07			
		STORM SEWER PIPE REINFORCED CONCRETE										STORM SEWER PIPE REINFORCED CONCRETE HORIZONTAL ELLIPTICAL								
		CLASS III	CLASS III	CLASS III	CLASS IV	CLASS IV	CLASS IV	CLASS IV	CLASS IV	CLASS IV	CLASS HE-III	CLASS HE-IV	CLASS HE-IV	CLASS HE-IV	CLASS HE-IV	CLASS HE-IV	CLASS HE-IV	CLASS HE-IV	JOINTTIES*	
FROM - TO	INLET ELEVATION	DISCHARGE ELEVATION	SLOPE FT/FT	15-INCH LF	30-INCH LF	36-INCH LF	12-INCH LF	15-INCH LF	18-INCH LF	21-INCH LF	24-INCH LF	30-INCH LF	43X68-INCH LF	19X30-INCH LF	24X38-INCH LF	34X53-INCH LF	38X60-INCH LF	43X68-INCH LF	58X91-INCH LF	EACH
71A - 71B	838.85	838.48	0.0100	-	-	-	37	-	-	-	-	-	-	-	-	-	-	-	-	-
71B - 71	838.48	838.41	0.0100	-	-	-	7	-	-	-	-	-	-	-	-	-	-	-	-	-
71C - 71D	838.82	838.72	0.0075	-	-	-	13	-	-	-	-	-	-	-	-	-	-	-	-	-
71D - 71	838.72	838.38	0.0100	-	-	-	34	-	-	-	-	-	-	-	-	-	-	-	-	-
72 - EX-8	834.61	834.47	0.0042	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	34	-
72A - 72D	834.79	834.77	0.0047	-	-	-	-	-	-	-	-	-	-	-	4	-	-	-	-	-
72B - 72A	838.96	838.62	0.0100	-	-	-	34	-	-	-	-	-	-	-	-	-	-	-	-	-
72C - 72A	834.82	834.79	0.0071	-	4	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
73 - 72	837.95	837.66	0.0075	-	-	-	-	-	-	38	-	-	-	-	-	-	-	-	-	-
73A - 73	839.00	838.95	0.0075	-	-	-	7	-	-	-	-	-	-	-	-	-	-	-	-	-
74 - 73	840.00	837.95	0.0075	-	-	-	-	-	-	273	-	-	-	-	-	-	-	-	-	-
74A - 74	841.30	841.00	0.0075	-	-	-	40	-	-	-	-	-	-	-	-	-	-	-	-	-
75 - 74	842.29	840.00	0.0075	-	-	-	-	-	-	305	-	-	-	-	-	-	-	-	-	-
75A - 75	843.49	843.29	0.0050	-	-	-	40	-	-	-	-	-	-	-	-	-	-	-	-	-
76 - 75	845.11	842.79	0.0075	-	-	-	-	-	309	-	-	-	-	-	-	-	-	-	-	-
76A - 76	845.78	845.61	0.0050	-	-	-	34	-	-	-	-	-	-	-	-	-	-	-	-	-
77 - 76	848.45	845.36	0.0100	-	-	-	-	309	-	-	-	-	-	-	-	-	-	-	-	-
78 - 77	849.87	849.70	0.0100	-	-	-	17	-	-	-	-	-	-	-	-	-	-	-	-	-
78A - 78	850.03	849.87	0.0100	-	-	-	16	-	-	-	-	-	-	-	-	-	-	-	-	-
78B - 78	850.03	849.87	0.0100	-	-	-	16	-	-	-	-	-	-	-	-	-	-	-	-	-
79 - 77	850.76	850.20	0.0100	-	-	-	56	-	-	-	-	-	-	-	-	-	-	-	-	-
79A - 79	850.95	850.76	0.0100	-	-	-	19	-	-	-	-	-	-	-	-	-	-	-	-	-
79B - 79	850.95	850.76	0.0100	-	-	-	20	-	-	-	-	-	-	-	-	-	-	-	-	-
90 - 91	847.80	841.14	0.0210	-	-	-	-	317	-	-	-	-	-	-	-	-	-	-	-	-
90A - 90B	848.45	848.12	0.0121	-	-	-	27	-	-	-	-	-	-	-	-	-	-	-	-	-
90B - 90	848.12	848.05	0.0100	-	-	-	7	-	-	-	-	-	-	-	-	-	-	-	-	-
90C - 90	848.57	848.05	0.0153	-	-	-	34	-	-	-	-	-	-	-	-	-	-	-	-	-
91 - 92	841.14	821.56	0.0600	-	-	-	-	327	-	-	-	-	-	-	-	-	-	-	-	-
91A - 91	843.46	843.39	0.0100	-	-	-	7	-	-	-	-	-	-	-	-	-	-	-	-	-
91B - 91	843.73	843.39	0.0100	-	-	-	34	-	-	-	-	-	-	-	-	-	-	-	-	-
92 - 202	821.56	819.86	0.0649	-	-	-	-	26	-	-	-	-	-	-	-	-	-	-	-	-
92A - 92	821.88	821.81	0.0100	-	-	-	7	-	-	-	-	-	-	-	-	-	-	-	-	-
92B - 92	823.48	823.14	0.0100	-	-	-	34	-	-	-	-	-	-	-	-	-	-	-	-	-
93 - 94	809.44	806.40	0.0075	-	-	405	-	-	-	-	-	-	-	-	-	-	-	-	-	-
93A - 93B	815.58	815.30	0.0100	-	-	-	28	-	-	-	-	-	-	-	-	-	-	-	-	-
93B - 293	815.05	814.84	0.0100	-	-	-	-	21	-	-	-	-	-	-	-	-	-	-	-	-
93C - 293	814.92	814.84	0.0100	-	-	-	-	8	-	-	-	-	-	-	-	-	-	-	-	-
93D - 93C	815.12	814.92	0.0100	-	-	-	-	20	-	-	-	-	-	-	-	-	-	-	-	-
93E - 93F	816.40	816.30	0.0100	-	-	-	10	-	-	-	-	-	-	-	-	-	-	-	-	-
93F - 93	816.30	815.96	0.0100	-	-	-	34	-	-	-	-	-	-	-	-	-	-	-	-	-
93G - 93C	815.27	815.17	0.0100	-	-	-	10	-	-	-	-	-	-	-	-	-	-	-	-	-
93H - 93B	815.41	815.30	0.0100	-	-	-	10	-	-	-	-	-	-	-	-	-	-	-	-	-
94 - 95	806.40	806.12	0.0100	-	-	28	-	-	-	-	-	-	-	-	-	-	-	-	-	-
94A - 94B	814.78	814.68	0.0100	-	-	-	10	-	-	-	-	-	-	-	-	-	-	-	-	-
94B - 94	814.43	813.70	0.0215	-	-	-	-	34	-	-	-	-	-	-	-	-	-	-	-	-
SUBTOTAL				-	4	433	642	1,062	309	-	616	-	-	-	4	-	-	-	34	-

*NON-BID ITEM: FOR INFORMATION ONLY

STORM SEWER

		608.0315	608.0330	608.0336	608.0412	608.0415	608.0418	608.0421	608.0424	608.0430	608.2343	608.2419	608.2424	608.2434	SPV.0090.05	SPV.0090.06	SPV.0090.07				
		STORM SEWER PIPE REINFORCED CONCRETE										STORM SEWER PIPE REINFORCED CONCRETE HORIZONTAL ELLIPTICAL									
		CLASS III	CLASS III	CLASS III	CLASS IV	CLASS IV	CLASS IV	CLASS IV	CLASS IV	CLASS IV	CLASS HE-III	CLASS HE-IV	CLASS HE-IV	CLASS HE-IV	CLASS HE-IV	CLASS HE-IV	CLASS HE-IV	CLASS HE-IV	JOINTTIES*		
FROM - TO	INLET ELEVATION	DISCHARGE ELEVATION	SLOPE FT/FT	15-INCH LF	30-INCH LF	36-INCH LF	12-INCH LF	15-INCH LF	18-INCH LF	21-INCH LF	24-INCH LF	30-INCH LF	43X68-INCH LF	19X30-INCH LF	24X38-INCH LF	34X53-INCH LF	38X60-INCH LF	43X68-INCH LF	58X91-INCH LF	EACH	
95 - 96	806.12	802.72	0.0536	-	-	64	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
95A - 95	814.47	814.40	0.0100	-	-	-	7	-	-	-	-	-	-	-	-	-	-	-	-	-	-
96 - 96W	801.10	801.03	0.0050	-	-	14	-	-	-	-	-	-	-	-	-	-	-	-	-	-	4
97 - 98	811.03	810.00	0.0305	34	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
98 - 99	810.00	804.60	0.0550	98	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
99 - 99W	804.27	804.23	0.0050	8	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	2
100A - 100	843.01	842.79	0.0098	-	-	-	23	-	-	-	-	-	-	-	-	-	-	-	-	-	-
100B - 100	842.84	842.79	0.0053	-	-	-	9	-	-	-	-	-	-	-	-	-	-	-	-	-	-
110 - 111	809.86	806.71	0.0100	-	-	-	-	315	-	-	-	-	-	-	-	-	-	-	-	-	-
110A - 110	812.41	812.32	0.0100	-	-	-	9	-	-	-	-	-	-	-	-	-	-	-	-	-	-
110B - 110	812.58	812.32	0.0100	-	-	-	26	-	-	-	-	-	-	-	-	-	-	-	-	-	-
111 - 112	806.71	806.59	0.0050	-	-	-	-	24	-	-	-	-	-	-	-	-	-	-	-	-	-
111A - 111	807.03	806.96	0.0035	-	-	-	20	-	-	-	-	-	-	-	-	-	-	-	-	-	-
112 - 120	806.59	806.32	0.0665	-	-	-	-	4	-	-	-	-	-	-	-	-	-	-	-	-	-
112A - 112	806.87	806.78	0.0100	-	-	-	9	-	-	-	-	-	-	-	-	-	-	-	-	-	-
200 - 201	816.87	816.38	0.0491	-	-	-	-	-	-	-	10	-	-	-	-	-	-	-	-	-	-
201 - 202	816.38	815.61	0.0134	-	-	-	-	-	-	-	58	-	-	-	-	-	-	-	-	-	-
201A - 201B	819.09	818.99	0.0100	-	-	-	10	-	-	-	-	-	-	-	-	-	-	-	-	-	-
201B - 201	818.99	818.88	0.0082	-	-	-	13	-	-	-	-	-	-	-	-	-	-	-	-	-	-
201C - 201D	819.24	819.14	0.0100	-	-	-	10	-	-	-	-	-	-	-	-	-	-	-	-	-	-
201D - 201	819.14	818.88	0.0122	-	-	-	21	-	-	-	-	-	-	-	-	-	-	-	-	-	-
202 - 93	812.25	809.94	0.0116	-	199	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
274 - 74	841.00	840.75	0.0100	-	-	-	-	25	-	-	-	-	-	-	-	-	-	-	-	-	-
274A - 274	841.41	841.30	0.0050	-	-	-	22	-	-	-	-	-	-	-	-	-	-	-	-	-	-
274B - 274C	841.44	841.34	0.0053	-	-	-	19	-	-	-	-	-	-	-	-	-	-	-	-	-	-
274C - 274	841.09	841.04	0.0053	-	-	-	-	9	-	-	-	-	-	-	-	-	-	-	-	-	-
275 - 75	843.10	842.99	0.0049	-	-	-	-	22	-	-	-	-	-	-	-	-	-	-	-	-	-
275A - 275	843.51	843.40	0.0050	-	-	-	23	-	-	-	-	-	-	-	-	-	-	-	-	-	-
275B - 275C	843.55	843.45	0.0050	-	-	-	21	-	-	-	-	-	-	-	-	-	-	-	-	-	-
275C - 275	843.45	843.35	0.0099	-	-	-	10	-	-	-	-	-	-	-	-	-	-	-	-	-	-
276 - 76	845.47	845.36	0.0049	-	-	-	-	23	-	-	-	-	-	-	-	-	-	-	-	-	-
276A - 276	845.83	845.72	0.0050	-	-	-	21	-	-	-	-	-	-	-	-	-	-	-	-	-	-
276B - 276C	845.86	845.77	0.0047	-	-	-	18	-	-	-	-	-	-	-	-	-	-	-	-	-	-
276C - 276	845.77	845.72	0.0057	-	-	-	9	-	-	-	-	-	-	-	-	-	-	-	-	-	-
293 - 93	814.84	814.52	0.0125	-	-	-	-	26	-	-	-	-	-	-	-	-	-	-	-	-	-
SUBTOTAL					140	199	78	300	448	-	-	-	68	-	-	-	-	-	-	-	6
TOTAL 0010					140	203	511	2,072	1,714	679	35	954	272	598	71	183	318	564	785	34	18

*NON-BID ITEM: FOR INFORMATION ONLY

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STORM SEWER

STRUCTURE	STATION	OFFSET*	INVERT ELEVATION	522.1015	522.1030	522.1036	522.2643
				APRON ENDWALLS FOR CULVERT PIPE REINFORCED CONCRETE 15-INCH EACH	APRON ENDWALLS FOR CULVERT PIPE REINFORCED CONCRETE 30-INCH EACH	APRON ENDWALLS FOR CULVERT PIPE REINFORCED CONCRETE 36-INCH EACH	APRON ENDWALLS FOR CULVERT PIPE REINFORCED CONCRETE HORIZONTAL ELLIPTICAL 43X68-INCH EACH
12W	104+20.52	52.12' RT	839.50				1
19W	104+70.14	54.38' RT	839.50		1		
96W	164+41.02	83.62' LT	801.03			1	
99W	164+49.14	78.52' LT	804.23	1			
TOTAL 0010				1	1	1	1

REMARKS:
 *STATIONS AND OFFSETS ARE TO CENTER OF STRUCTURE

STORM SEWER

STRUCTURE	STATION	OFFSET*	RIM**	INVERT***	DEPTH****	520.8000	611.0624	611.1004	611.1005	611.1006	611.1230	611.3003	611.8105	611.8110	612.0406	SPV.006.08
			ELEVATION	ELEVATION	FT	CONCRETE	INLET	CATCH	CATCH	CATCH	CATCH	ADJUSTING	ADJUSTING	PIPE	INLET	
						COLLARS	COVERS	BASINS 4-FT	BASINS 5-FT	BASINS 6-FT	BASINS	INLETS 3-FT	BASIN	INLET COVERS	UNDERDRAIN	COVERS
			FOR PIPE	TYPE H	DIAMETER	DIAMETER	DIAMETER	2X3-FT	DIAMETER	COVERS	COVERS	6-INCH	SPECIAL			
			EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	EACH	LF	EACH		
1	100+21.24	7.20' RT	847.78	841.61	4.92	-	-	-	-	-	-	-	-	-	-	-
10	102+50.00	5.50' RT	846.39	840.48	4.66	-	-	-	-	-	-	-	-	-	-	-
10A	102+50.00	29.50' LT	845.94	840.20	4.74	-	1	1	-	-	-	-	-	-	50	-
10B	102+50.00	17.50' RT	846.18	841.12	4.06	-	1	-	-	-	1	-	-	-	50	-
11	103+25.00	5.50' RT	846.10	840.10	4.75	-	-	-	-	-	-	-	-	-	-	-
11A	103+21.33	27.98' LT	845.68	840.86	3.82	-	1	-	-	-	1	-	-	-	50	-
11B	103+21.33	17.50' RT	845.89	840.75	4.14	-	1	-	-	-	1	-	-	-	100	-
12A	103+63.57	40.38' RT	845.49	839.39	5.10	-	1	-	-	-	1	-	-	-	60	-
12B	103+63.60	55.11' RT	845.19	839.29	4.90	-	1	-	-	-	1	-	-	-	-	-
12C	103+97.50	55.16' RT	845.21	838.48	5.73	-	1	-	-	-	1	-	-	-	-	-
12D	103+97.64	40.66' RT	845.62	837.70	6.67	-	-	-	-	-	-	-	-	-	60	-
21	105+15.00	10.49' LT	845.90	839.69	4.96	-	-	-	-	-	-	-	-	-	-	-
22	106+94.18	10.50' LT	844.10	839.94	2.99	-	-	-	-	-	-	-	-	-	-	-
23	106+95.99	47.92' LT	843.79	840.11	2.43	1	-	-	-	-	-	-	-	-	-	-
23A	106+60.00	23.50' LT	843.92	838.68	4.24	-	1	1	-	-	-	-	-	-	50	-
23B	106+87.60	47.91' LT	843.59	838.62	3.97	-	1	1	-	-	-	-	-	-	38	-
23C	107+23.03	47.97' LT	843.83	838.71	4.12	-	1	-	-	-	1	-	-	-	50	-
24	107+65.00	16.50' LT	843.69	840.06	2.55	-	-	-	-	-	-	-	-	-	-	-
24A	107+50.00	23.50' LT	843.53	838.49	4.04	-	1	-	-	-	1	-	-	-	38	-
24B	107+65.00	23.50' LT	843.55	838.42	4.13	-	1	-	-	-	1	-	-	-	60	-
24C	107+75.22	17.50' RT	843.77	838.80	3.97	-	1	-	-	-	1	-	-	-	-	-
24D	107+65.00	17.50' RT	843.78	838.77	4.01	-	1	-	-	-	1	-	-	-	60	-
24W	107+65.00	34.56' RT	843.00	840.86	1.31	-	-	-	-	-	-	1	-	-	-	1
25	110+75.00	16.50' LT	845.12	840.84	3.03	-	-	-	-	-	-	-	-	-	-	-
25A	110+75.00	23.50' LT	845.01	839.41	4.60	-	1	-	-	-	1	-	-	-	50	-
25B	110+75.00	17.50' RT	845.13	839.68	4.45	-	1	-	-	-	1	-	-	-	50	-
26	112+00.01	16.50' LT	845.74	841.40	3.09	-	-	-	-	-	-	-	-	-	-	-
26W	112+60.00	35.14' RT	845.45	842.45	2.17	-	-	-	-	-	-	1	-	-	-	1
30	113+69.49	44.60' LT	846.78	841.70	3.91	1	-	-	-	-	-	-	-	-	-	-
30A	113+61.27	46.01' LT	846.77	840.79	4.98	-	1	-	-	-	1	-	-	-	8	-
30B	113+97.02	46.81' LT	846.61	840.98	4.63	-	1	-	-	-	1	-	-	-	8	-
31	113+68.91	16.50' LT	846.48	841.45	3.78	-	-	-	-	-	-	-	-	-	-	-
32	116+25.00	16.50' LT	844.32	839.01	4.06	-	-	-	-	-	-	-	-	-	-	-
32A	116+25.00	23.50' LT	844.21	838.25	4.96	-	1	-	-	-	1	-	-	-	50	-
32B	116+25.00	17.50' RT	844.25	838.52	4.73	-	1	-	-	-	1	-	-	-	50	-
39	117+18.76	17.50' LT	843.76	838.67	3.84	-	-	-	-	-	-	-	-	-	-	-
40	117+19.76	47.99' LT	843.77	839.12	3.40	1	-	-	-	-	-	-	-	-	-	-
40A	117+03.64	28.26' LT	843.54	837.88	4.66	-	1	1	-	-	-	-	-	-	72	-
40B	117+12.46	48.34' LT	843.63	837.77	4.86	-	1	1	-	-	-	-	-	-	-	-
40C	117+46.57	46.76' LT	843.67	837.76	4.91	-	1	1	-	-	-	-	-	-	-	-
40D	117+48.57	28.59' LT	843.36	837.85	4.51	-	1	1	-	-	-	-	-	-	32	-
41	118+00.00	17.50' LT	843.62	838.49	3.96	-	-	-	-	-	-	-	-	-	-	-
41B	118+00.00	23.50' LT	843.53	837.76	4.77	-	1	-	-	-	1	-	-	-	82	-
41C	117+77.50	17.50' RT	843.55	837.87	4.68	-	1	1	-	-	-	-	-	-	56	-
41D	117+67.50	17.50' RT	843.55	837.92	4.63	-	1	-	-	-	1	-	-	-	50	-
SUBTOTAL						3	27	8	-	-	19	2	-	-	1174	2

REMARKS:

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ADJUSTMENT HEIGHT IS 6 INCHES UNLESS OTHERWISE NOTED

STORM SEWER

STRUCTURE	STATION	OFFSET*	RIM**	INVERT***	DEPTH****	520.8000	611.0624	611.1004	611.1005	611.1006	611.1230	611.3003	611.8105	611.8110	612.0406	SPV.006.08
			ELEVATION	ELEVATION	FT	CONCRETE COLLARS FOR PIPE EACH	INLET COVERS TYPE H EACH	CATCH BASINS 4-FT DIAMETER EACH	CATCH BASINS 5-FT DIAMETER EACH	CATCH BASINS 6-FT DIAMETER EACH	CATCH BASINS 2X3-FT EACH	INLETS 3-FT DIAMETER EACH	ADJUSTING CATCH BASIN COVERS EACH	ADJUSTING INLET COVERS COVERS EACH	PIPE UNDERDRAIN WRAPPED 6-INCH LF	INLET COVERS SPECIAL EACH
44	120+36.68	48.80' LT	843.05	838.34	3.46	1	-	-	-	-	-	-	-	-	-	-
44A	120+07.35	23.50' LT	843.18	838.04	4.14	-	1	1	-	-	-	-	-	-	50	-
44B	120+29.39	48.23' LT	842.85	837.63	4.22	-	1	1	-	-	-	-	-	-	32	-
44C	120+64.56	47.73' LT	842.87	837.98	3.89	-	1	-	-	-	1	-	-	-	23	-
45	120+36.24	17.50' LT	843.32	838.02	4.30	-	-	-	-	-	-	-	-	-	-	-
45A	120+35.78	17.50' RT	843.43	837.86	4.57	-	1	-	-	-	1	-	-	-	50	-
50	122+10.00	16.50' LT	842.74	837.68	4.31	-	-	-	-	-	-	-	-	-	50	-
50A	122+00.00	23.50' LT	842.62	837.42	4.20	-	1	-	-	-	1	-	-	-	-	-
50B	122+10.00	23.50' LT	842.63	837.32	4.31	-	1	-	-	-	1	-	-	-	50	-
50C	122+10.00	17.50' RT	842.75	837.42	4.33	-	1	-	-	-	1	-	-	-	56	-
50D	122+00.00	17.50' RT	842.74	837.47	4.27	-	1	-	-	-	1	-	-	-	56	-
55	123+36.11	16.50' LT	843.28	837.40	4.63	-	-	-	-	-	-	-	-	-	-	-
55A	123+35.54	17.50' RT	843.29	837.91	4.38	-	1	-	-	-	1	-	-	-	50	-
56	123+36.64	47.69' LT	843.06	837.84	3.97	1	-	-	-	-	-	-	-	-	-	-
56A	123+29.04	47.88' LT	842.88	836.46	5.42	-	1	-	-	-	1	-	-	-	25	-
56B	123+64.15	47.18' LT	842.96	836.86	5.10	-	1	-	-	-	1	-	-	-	50	-
57	124+91.73	16.50' LT	843.69	837.17	5.27	-	-	-	-	-	-	-	-	-	-	-
57A	124+91.73	52.56' LT	841.23	836.31	4.09	-	-	-	-	-	-	1	-	-	-	1
60	125+99.92	16.50' LT	842.99	837.01	4.81	-	-	-	-	-	-	-	-	-	-	-
60A	126+17.90	26.79' LT	842.83	837.10	4.73	-	1	1	-	-	-	-	-	-	20	-
60B	125+99.92	23.50' LT	842.88	836.96	4.92	-	1	-	-	-	1	-	-	-	50	-
60C	126+05.00	17.50' RT	842.84	837.17	4.67	-	1	-	-	-	1	-	-	-	65	-
60D	126+25.18	17.50' RT	842.89	837.32	4.57	-	1	-	-	-	1	-	-	-	50	-
60E	126+27.21	42.67' LT	843.05	837.24	4.81	-	1	1	-	-	-	-	-	-	-	-
61	126+65.88	16.50' LT	843.24	836.91	5.08	1	-	-	-	-	-	-	-	-	-	-
62	126+84.01	16.50' LT	843.33	836.87	5.21	-	-	-	-	-	-	-	-	-	-	-
62A	126+84.01	28.52' LT	842.91	837.57	4.34	-	1	1	-	-	-	-	-	-	65	-
63	129+71.29	39.69' LT	844.70	840.53	2.92	-	-	-	-	-	-	-	-	-	-	-
63A	129+42.08	28.25' LT	844.36	838.92	4.44	-	1	1	-	-	-	-	-	-	13	-
63B	129+49.44	43.38' LT	844.62	838.75	4.87	-	1	1	-	-	-	-	-	-	-	-
63C	129+86.85	28.90' LT	844.39	838.81	4.58	-	1	1	-	-	-	-	-	-	13	-
63D	129+80.20	45.22' LT	844.69	838.63	5.06	-	1	1	-	-	-	-	-	-	-	-
64	129+74.29	16.50' LT	844.66	836.38	7.03	-	-	-	-	-	-	-	-	-	-	-
70	132+46.89	16.50' LT	843.62	835.57	6.80	-	-	-	-	-	-	-	-	-	-	-
70A	132+82.71	45.82' LT	843.39	837.53	4.86	-	1	-	-	-	1	-	-	-	20	-
70B	132+49.18	43.87' LT	843.57	837.36	5.21	-	1	1	-	-	-	-	-	-	21	-
70C	132+40.00	26.91' LT	843.38	837.17	5.21	-	1	1	-	-	-	-	-	-	65	-
70D	132+50.00	17.50' RT	843.54	837.38	5.16	-	1	-	-	-	1	-	-	-	50	-
71	134+16.00	16.50' LT	842.83	835.07	6.51	-	-	-	-	-	-	-	-	-	-	-
71A	133+78.68	23.50' LT	842.76	836.85	4.91	-	1	-	-	-	1	-	-	-	50	-
71B	134+16.00	23.50' RT	842.69	836.48	5.21	-	1	-	-	-	1	-	-	-	-	-
71C	134+02.53	17.50' RT	842.85	836.82	5.03	-	1	-	-	-	1	-	-	-	50	-
71D	134+16.00	17.50' RT	842.86	836.72	5.14	-	1	-	-	-	1	-	-	-	59	-
72	135+72.73	18.22' LT	842.91	834.61	7.05	-	-	-	-	-	-	-	-	-	-	-
72A	135+80.97	17.50' RT	842.93	834.79	7.14	2	1	-	1	-	-	-	-	-	78	-
SUBTOTAL						5	30	11	1	-	18	1	-	-	1,161	1

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STORM SEWER

STRUCTURE	STATION	OFFSET*	RIM**	INVERT***	DEPTH****	520.8000	611.0624	611.1004	611.1005	611.1006	611.1230	611.3003	611.8105	611.8110	612.0406	SPV.006.08
			ELEVATION	ELEVATION	FT	CONCRETE COLLARS FOR PIPE EACH	INLET COVERS TYPE H EACH	CATCH BASINS 4-FT DIAMETER EACH	CATCH BASINS 5-FT DIAMETER EACH	CATCH BASINS 6-FT DIAMETER EACH	CATCH BASINS 2X3-FT EACH	INLETS 3-FT DIAMETER EACH	ADJUSTING CATCH BASIN COVERS EACH	ADJUSTING INLET COVERS COVERS EACH	PIPE UNDERDRAIN WRAPPED 6-INCH LF	INLET COVERS SPECIAL EACH
72B	136+15.00	17.50' RT	842.80	836.96	4.84	-	1	-	-	-	1	-	-	-	50	-
73	136+11.11	16.50' LT	842.87	837.95	3.67	-	-	-	-	-	-	-	-	-	-	-
73A	136+11.11	23.50' LT	842.76	837.00	4.76	-	1	-	-	-	1	-	-	-	50	-
74	138+84.30	16.50' LT	844.29	840.00	3.04	-	-	-	-	-	-	-	-	-	-	-
74A	139+05.10	17.50' RT	844.34	839.30	4.04	-	1	1	-	-	-	-	-	-	50	-
75	141+88.45	16.50' LT	846.49	842.29	2.95	-	-	-	-	-	-	-	-	-	-	-
75A	142+10.00	17.50' RT	846.67	841.49	4.18	-	1	1	-	-	-	-	-	-	50	-
76	144+97.94	16.50' LT	849.49	845.11	3.13	-	-	-	-	-	-	-	-	-	-	-
76A	144+97.94	17.50' RT	849.42	843.78	4.64	-	1	-	-	-	1	-	-	-	50	-
77	148+06.75	16.50' LT	854.46	848.45	4.76	-	-	-	-	-	-	-	-	-	-	-
78	148+06.94	33.80' LT	854.39	849.87	3.27	-	-	-	-	-	-	-	-	-	-	-
78A	147+92.37	40.99' LT	854.23	848.03	5.20	-	1	-	-	-	1	-	-	-	29	-
78B	148+21.49	40.98' LT	854.21	848.03	5.18	-	1	-	-	-	1	-	-	-	29	-
79	148+06.15	39.92' RT	855.06	850.42	3.39	-	-	-	-	-	-	-	-	-	-	-
79A	147+86.77	39.81' RT	854.43	848.95	4.48	-	1	-	-	-	1	-	-	-	-	-
79B	148+25.68	40.12' RT	854.73	848.95	4.78	-	1	-	-	-	1	-	-	-	-	-
90	150+91.66	16.50' LT	852.80	847.80	3.75	-	-	-	-	-	-	-	-	-	-	-
90A	151+12.00	41.62' LT	852.27	846.45	4.82	-	1	1	-	-	-	-	-	-	24	-
90B	150+91.66	23.50' LT	852.69	846.12	5.57	-	1	1	-	-	-	-	-	-	50	-
90C	150+91.66	17.50' RT	852.78	846.57	5.21	-	1	-	-	-	1	-	-	-	50	-
91	154+08.36	16.50' LT	847.75	841.14	5.36	-	-	-	-	-	-	-	-	-	-	-
91A	154+08.36	23.50' LT	847.64	841.46	5.18	-	1	-	-	-	1	-	-	-	50	-
91B	154+08.36	17.50' RT	847.60	841.73	4.87	-	1	-	-	-	1	-	-	-	50	-
92	157+34.66	16.50' LT	826.13	821.56	3.32	-	-	-	-	-	-	-	-	-	-	-
92A	157+34.66	23.50' LT	826.02	819.88	5.14	-	1	-	-	-	1	-	-	-	50	-
92B	157+31.85	17.50' RT	826.15	821.48	3.67	-	1	-	-	-	1	-	-	-	50	-
93	159+60.01	16.50' LT	819.65	809.44	8.96	-	-	-	-	-	-	-	-	-	-	-
93A	159+17.06	23.50' LT	819.74	813.58	5.16	-	1	1	-	-	-	-	-	-	50	-
93B	159+37.47	42.05' LT	819.36	813.05	5.31	-	1	1	-	-	-	-	-	-	31	-
93C	159+67.31	42.05' LT	819.38	812.92	5.46	-	1	1	-	-	-	-	-	-	21	-
93D	159+78.23	25.84' LT	819.56	813.12	5.44	-	1	1	-	-	-	-	-	-	50	-
93E	159+44.60	17.50' RT	819.55	814.40	4.15	-	1	-	-	-	1	-	-	-	50	-
93F	159+54.60	17.50' RT	819.50	814.30	4.20	-	1	-	-	-	1	-	-	-	56	-
93G	159+67.17	52.05' LT	819.43	813.27	5.16	-	1	-	-	-	1	-	-	-	-	-
93H	159+37.53	52.05' LT	819.42	813.41	5.01	-	1	-	-	-	1	-	-	-	-	-
94	163+65.00	16.50' LT	818.98	806.40	11.33	-	-	-	-	-	-	-	-	-	50	-
94A	163+75.00	17.50' RT	818.79	812.78	5.01	-	1	-	-	-	1	-	-	-	-	-
94B	163+65.00	17.50' RT	818.83	812.43	5.40	-	1	-	-	-	1	-	-	-	55	-
95	163+92.57	23.50' LT	818.78	804.12	13.66	-	1	-	-	1	-	-	-	-	-	-
95A	164+00.00	23.50' LT	818.78	812.47	5.31	-	1	-	-	-	1	-	-	-	50	-
96	164+34.22	71.32' LT	809.34	801.10	6.99	-	-	-	-	-	-	-	-	-	-	-
97	163+63.49	28.91' RT	816.63	811.03	4.35	-	-	-	-	-	-	-	-	-	-	-
98	163+92.19	11.00' RT	818.97	810.00	7.72	-	-	-	-	-	-	-	-	-	50	-
99	164+45.65	71.32' LT	809.25	804.27	3.73	-	-	-	-	-	-	-	-	-	-	-
100	154+53.23	45.38' LT	846.42	-	-	-	-	-	-	-	-	-	-	-	-	-
SUBTOTAL						-	28	8	-	1	19	-	-	-	1,095	-

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STRUCTURE	STATION	OFFSET*	RIM**	INVERT***	DEPTH****	520.8000	611.0624	611.1004	611.1005	611.1006	611.1230	611.3003	611.8105	611.8110	612.0406	SPV.006.08
			ELEVATION	ELEVATION	FT	CONCRETE COLLARS FOR PIPE EACH	INLET COVERS TYPE H EACH	CATCH BASINS 4-FT DIAMETER EACH	CATCH BASINS 5-FT DIAMETER EACH	CATCH BASINS 6-FT DIAMETER EACH	CATCH BASINS 2X3-FT EACH	INLETS 3-FT DIAMETER EACH	CATCH BASIN COVERS EACH	ADJUSTING INLET COVERS EACH	UNDERDRAIN PIPE WRAPPED 6-INCH LF	INLET COVERS SPECIAL EACH
100A	154+31.05	41.57'LT	846.43	-	-	-	-	-	-	-	-	-	1	-	27	-
100B	154+62.15	42.19'LT	846.04	-	-	-	-	-	-	-	-	-	1	-	-	-
110	169+50.00	8.50'LT	816.48	809.86	5.37	-	-	-	-	-	-	-	-	-	-	-
110A	169+50.00	17.50'LT	816.33	810.41	4.92	-	1	-	-	-	1	-	-	-	50	-
110B	169+50.00	17.50'RT	816.97	810.58	5.39	-	1	-	-	-	1	-	-	-	-	-
111	172+64.49	8.50'LT	810.28	806.71	2.32	-	-	-	-	-	-	-	-	-	-	-
111A	172+81.12	20.06'LT	809.68	805.03	3.73	-	1	1	-	-	-	-	-	-	50	-
112	172+81.57	8.66'RT	810.00	806.59	2.16	1	-	-	-	-	-	-	-	-	-	-
112A	172+78.62	17.50'RT	810.05	804.87	4.18	-	1	-	-	-	1	-	-	-	50	-
201	157+67.22	40.76'RT	823.71	816.38	6.08	1	-	-	-	-	-	-	-	-	-	-
201A	157+53.63	50.75'RT	823.71	817.09	5.62	-	1	-	-	-	1	-	-	-	-	-
201B	157+53.81	40.76'RT	823.61	816.99	5.62	-	1	1	-	-	-	-	-	-	37	-
201C	157+88.54	50.76'RT	823.62	817.24	5.38	-	1	-	-	-	1	-	-	-	-	-
201D	157+88.45	40.76'RT	823.42	817.14	5.28	-	1	-	-	-	1	-	-	-	6	-
202	157+60.91	16.50'LT	824.68	812.25	11.18	-	-	-	-	-	-	-	-	-	-	-
274	138+84.44	41.65'LT	844.37	841.00	2.12	-	-	-	-	-	-	-	-	-	-	-
274A	138+63.42	47.01'LT	844.20	839.41	3.79	-	1	-	-	-	1	-	-	-	8	-
274B	138+99.46	28.75'LT	843.98	839.44	3.71	-	1	1	-	-	-	-	-	-	64	-
274C	138+92.71	46.06'LT	844.38	839.09	4.29	-	1	1	-	-	-	-	-	-	8	-
275	141+88.45	38.82'LT	846.77	842.76	2.76	-	-	-	-	-	-	-	-	-	-	-
275A	141+67.47	43.29'LT	846.68	841.51	4.17	-	1	-	-	-	1	-	-	-	-	-
275B	142+09.45	26.35'LT	846.30	841.55	3.75	-	1	1	-	-	-	-	-	-	66	-
275C	141+96.82	43.65'LT	846.83	841.45	4.38	-	1	1	-	-	-	-	-	-	-	-
276	144+97.94	39.13'LT	849.60	845.13	3.22	-	-	-	-	-	-	-	-	-	-	-
276A	144+76.67	41.03'LT	849.26	843.83	4.43	-	1	-	-	-	1	-	-	-	-	-
276B	145+16.73	26.09'LT	849.59	843.86	4.73	-	1	1	-	-	-	-	-	-	64	-
276C	145+06.38	41.39'LT	849.67	843.77	4.90	-	1	1	-	-	-	-	-	-	-	-
293	159+58.91	42.05'LT	819.51	814.84	3.42	-	-	-	-	-	-	-	-	-	-	-
EX-8	135+72.63	52.61'LT	842.93	-	-	-	-	-	-	-	-	-	-	-	-	-
EX-8A	135+50.33	49.33'LT	842.66	-	-	-	-	-	-	-	-	-	1	-	-	-
EX-8B	135+83.16	49.84'LT	842.62	-	-	-	-	-	-	-	-	-	-	1	-	-
EX-72	135+79.97	32.48'RT	842.64	-	-	-	-	-	-	-	-	-	-	1	-	-
SUBTOTAL						2	17	8	-	-	9	-	3	2	430	-
TOTAL 0010						10	102	35	1	1	65	3	3	2	3,860	3

REMARKS:
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 **RIM ELEV IS AT THE INLET COVER FLANGE LOCATION
 ***FOR STRUCTURES WITH SUMPS, THE INVERT ELEVATION IS THE ELEVATION OF THE SUMP. FOR STRUCTURES WITHOUT SUMPS, THE INVERT ELEVATION IS THE ELEVATION OF THE LOWEST PIPE FLOW LINE
 ****DEPTH = RIM ELEV - TOP OF STRUCTURE BASE ELEV - COVER HEIGHT - ADJUSTMENT
 ADJUSTMENT HEIGHT IS 6 INCHES UNLESS OTHERWISE NOTED

STORM SEWER

STRUCTURE	STATION	OFFSET*	RIM** ELEVATION	INVERT*** ELEVATION	DEPTH**** FT	611.0530	611.0545	611.2003	611.2004	611.2005	611.2006	611.2008	611.2010	611.8110	650.4000	REMARKS
						MANHOLE COVERS TYPE J EACH	MANHOLE COVERS TYPE L EACH	MANHOLES 3-FT DIAMETER EACH	MANHOLES 4-FT DIAMETER EACH	MANHOLES 5-FT DIAMETER EACH	MANHOLES 6-FT DIAMETER EACH	MANHOLES 8-FT DIAMETER EACH	MANHOLES 10-FT DIAMETER EACH	ADJUSTING MANHOLE COVERS EACH	CONSTRUCTION STAKING STORM SEWER EACH	
1	100+21.24	7.20' RT	847.78	841.61	4.92	1	-	-	-	-	-	-	1	-	1	CENTER CASTING ON MANHOLE
10	102+50.00	5.50' RT	846.39	840.48	4.66	1	-	-	-	-	-	-	1	-	1	CENTER CASTING ON MANHOLE
10A	102+50.00	29.50' LT	845.94	840.20	4.74	-	-	-	-	-	-	-	-	-	1	-
10B	102+50.00	17.50' RT	846.18	841.12	4.06	-	-	-	-	-	-	-	-	-	1	-
11	103+25.00	5.50' RT	846.10	840.10	4.75	1	-	-	-	-	-	-	1	-	1	CENTER CASTING ON MANHOLE
11A	103+21.33	27.98' LT	845.68	840.86	3.82	-	-	-	-	-	-	-	-	-	1	-
11B	103+21.33	17.50' RT	845.89	840.75	4.14	-	-	-	-	-	-	-	-	-	1	-
12A	103+63.57	40.38' RT	845.49	839.39	5.10	-	-	-	-	-	-	-	-	-	1	-
12B	103+63.60	55.11' RT	845.19	839.29	4.90	-	-	-	-	-	-	-	-	-	1	-
12C	103+97.50	55.16' RT	845.21	838.48	5.73	-	-	-	-	-	-	-	-	-	1	-
12D	103+97.64	40.66' RT	845.62	837.70	6.67	1	-	-	-	-	-	-	1	-	1	-
21	105+15.00	10.49' LT	845.90	839.69	4.96	1	-	-	-	-	-	1	-	-	1	CENTER CASTING ON MANHOLE
22	106+94.18	10.50' LT	844.10	839.94	2.99	1	-	-	-	-	1	-	-	-	1	CENTER CASTING ON MANHOLE. 5 INCHES OF ADJUSTMENT.
23	106+95.99	47.92' LT	843.79	840.11	2.43	1	-	-	1	-	-	-	-	-	1	-
23A	106+60.00	23.50' LT	843.92	838.68	4.24	-	-	-	-	-	-	-	-	-	1	-
23B	106+87.60	47.91' LT	843.62	838.62	4.00	-	-	-	-	-	-	-	-	-	1	-
23C	107+23.03	47.97' LT	843.83	838.71	4.12	-	-	-	-	-	-	-	-	-	1	-
24	107+65.00	16.50' LT	843.69	840.06	2.55	1	-	-	-	1	-	-	-	-	1	POSITION STAIRS/CASTING OUT OF BIKE LANE. 4 INCHES OF ADJUSTMENT.
24A	107+50.00	23.50' LT	843.53	838.49	4.04	-	-	-	-	-	-	-	-	-	1	-
24B	107+65.00	23.50' LT	843.55	838.42	4.13	-	-	-	-	-	-	-	-	-	1	-
24C	107+75.22	17.50' RT	843.77	838.80	3.97	-	-	-	-	-	-	-	-	-	1	-
24D	107+65.00	17.50' RT	843.78	838.77	4.01	-	-	-	-	-	-	-	-	-	1	-
25	110+75.00	16.50' LT	845.12	840.84	3.03	1	-	-	1	-	-	-	-	-	1	POSITION STAIRS/CASTING OUT OF BIKE LANE.
25A	110+75.00	23.50' LT	845.01	839.41	4.60	-	-	-	-	-	-	-	-	-	1	-
25B	110+75.00	17.50' RT	845.13	839.68	4.45	-	-	-	-	-	-	-	-	-	1	-
26	112+00.01	16.50' LT	845.74	841.40	3.09	1	-	1	-	-	-	-	-	-	1	POSITION STAIRS/CASTING OUT OF BIKE LANE.
30	113+69.49	44.60' LT	846.78	841.70	3.91	1	-	-	1	-	-	-	-	-	1	5 INCHES OF ADJUSTMENT.
30A	113+61.27	46.01' LT	846.77	840.79	4.98	-	-	-	-	-	-	-	-	-	1	-
30B	113+97.02	46.81' LT	846.61	840.98	4.63	-	-	-	-	-	-	-	-	-	1	-
31	113+68.91	16.50' LT	846.48	841.45	3.78	1	-	-	-	1	-	-	-	-	1	POSITION STAIRS/CASTING OUT OF BIKE LANE.
32	116+25.00	16.50' LT	844.32	839.01	4.06	1	-	-	-	1	-	-	-	-	1	POSITION STAIRS/CASTING OUT OF BIKE LANE.
32A	116+25.00	23.50' LT	844.21	838.25	4.96	-	-	-	-	-	-	-	-	-	1	-
32B	116+25.00	17.50' RT	844.25	838.52	4.73	-	-	-	-	-	-	-	-	-	1	-
39	117+18.76	17.50' LT	843.76	838.67	3.84	1	-	-	-	-	-	1	-	-	1	POSITION STAIRS/CASTING OUT OF BIKE LANE.
40	117+19.76	47.99' LT	843.77	839.12	3.40	1	-	-	1	-	-	-	-	-	1	-
40A	117+03.64	28.26' LT	843.54	837.88	4.66	-	-	-	-	-	-	-	-	-	1	-
40B	117+12.46	48.34' LT	843.63	837.77	4.86	-	-	-	-	-	-	-	-	-	1	-
40C	117+46.57	46.76' LT	843.67	837.76	4.91	-	-	-	-	-	-	-	-	-	1	-
40D	117+48.57	28.59' LT	843.36	837.85	4.51	-	-	-	-	-	-	-	-	-	1	-
41	118+00.00	17.50' LT	843.62	838.49	3.96	1	-	-	-	-	-	-	-	-	1	5 INCHES OF ADJUSTMENT.
41B	118+00.00	23.50' LT	843.53	837.76	4.77	-	-	-	-	-	-	-	-	-	1	-
41C	117+77.50	17.50' RT	843.55	837.87	4.68	-	-	-	-	-	-	-	-	-	1	-
41D	117+67.50	17.50' RT	843.55	837.92	4.63	-	-	-	-	-	-	-	-	-	1	-
44	120+36.68	48.80' LT	843.05	838.34	3.46	1	-	-	1	-	-	-	-	-	1	-
44A	120+07.35	23.50' LT	843.18	838.04	4.14	-	-	-	-	-	-	-	-	-	1	-
SUBTOTAL						17	-	1	5	3	1	2	4	-	45	

REMARKS:
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STORM SEWER

STRUCTURE	STATION	OFFSET*	RIM** ELEVATION	INVERT*** ELEVATION	DEPTH**** FT	611.0530	611.0545	611.2003	611.2004	611.2005	611.2006	611.2008	611.2010	611.8110	650.4000	REMARKS
						MANHOLE COVERS EACH	MANHOLE COVERS EACH	MANHOLES 3-FT DIAMETER EACH	MANHOLES 4-FT DIAMETER EACH	MANHOLES 5-FT DIAMETER EACH	MANHOLES 6-FT DIAMETER EACH	MANHOLES 8-FT DIAMETER EACH	MANHOLES 10-FT DIAMETER EACH	ADJUSTING MANHOLE COVERS EACH	CONSTRUCTION STAKING STORM SEWER EACH	
44B	120+29.39	48.23' LT	842.85	837.63	4.22	-	-	-	-	-	-	-	-	-	1	-
44C	120+64.56	47.73' LT	842.87	837.98	3.89	-	-	-	-	-	-	-	-	-	1	-
45	120+36.24	17.50' LT	843.32	838.02	4.30	1	-	-	-	-	-	1	-	-	1	POSITION STAIRS/CASTING OUT OF BIKE LANE. 3 INCHES OF ADJUSTMENT.
45A	120+35.78	17.50' RT	843.43	837.86	4.57	-	-	-	-	-	-	-	-	-	1	-
50	122+10.00	16.50' LT	842.74	837.68	4.31	-	1	-	-	-	-	1	-	-	1	POSITION STAIRS/CASTING OUT OF BIKE LANE. 5 INCHES OF ADJUSTMENT.
50A	122+00.00	23.50' LT	842.62	837.42	4.20	-	-	-	-	-	-	-	-	-	1	-
50B	122+10.00	23.50' LT	842.63	837.32	4.31	-	-	-	-	-	-	-	-	-	1	-
50C	122+10.00	17.50' RT	842.75	837.42	4.33	-	-	-	-	-	-	-	-	-	1	-
50D	122+00.00	17.50' RT	842.74	837.47	4.27	-	-	-	-	-	-	-	-	-	1	-
55	123+36.11	16.50' LT	843.28	837.40	4.63	1	-	-	-	-	-	1	-	-	1	POSITION STAIRS/CASTING OUT OF BIKE LANE.
55A	123+35.54	17.50' RT	843.29	837.91	4.38	-	-	-	-	-	-	-	-	-	1	-
56	123+36.64	47.69' LT	843.06	837.84	3.97	1	-	-	1	-	-	-	-	-	1	-
56A	123+29.04	47.88' LT	842.88	836.46	5.42	-	-	-	-	-	-	-	-	-	1	-
56B	123+64.15	47.18' LT	842.96	836.86	5.10	-	-	-	-	-	-	-	-	-	1	-
57	124+91.73	16.50' LT	843.69	837.17	5.27	1	-	-	-	-	-	1	-	-	1	POSITION STAIRS/CASTING OUT OF BIKE LANE.
57A	124+91.73	52.56' LT	841.23	836.31	3.67	-	-	-	-	-	-	-	-	-	1	-
60	125+99.92	16.50' LT	842.99	837.01	4.81	1	-	-	-	-	-	-	-	-	1	POSITION STAIRS/CASTING OUT OF BIKE LANE. 5 INCHES OF ADJUSTMENT.
60A	126+17.90	26.79' LT	842.83	837.10	4.73	-	-	-	-	-	-	-	-	-	1	-
60B	125+99.92	23.50' LT	842.88	836.96	4.92	-	-	-	-	-	-	-	-	-	1	-
60C	126+05.00	17.50' RT	842.84	837.17	4.67	-	-	-	-	-	-	-	-	-	1	-
60D	126+25.18	17.50' RT	842.89	837.32	4.57	-	-	-	-	-	-	-	-	-	1	-
60E	126+27.21	42.67' LT	843.05	837.24	4.81	-	-	-	-	-	-	-	-	-	1	-
61	126+65.88	16.50' LT	843.24	836.91	5.08	1	-	-	-	-	-	-	1	-	1	CENTER CASTING ON MANHOLE.
62	126+84.01	16.50' LT	843.33	836.87	5.21	1	-	-	-	-	-	-	1	-	1	CENTER CASTING ON MANHOLE.
62A	126+84.01	28.52' LT	842.91	837.57	4.34	-	-	-	-	-	-	-	-	-	1	-
63	129+71.29	39.69' LT	844.70	840.53	2.92	1	-	1	-	-	-	-	-	-	1	-
63A	129+42.08	28.25' LT	844.36	838.92	4.44	-	-	-	-	-	-	-	-	-	1	-
63B	129+49.44	43.38' LT	844.62	838.75	4.87	-	-	-	-	-	-	-	-	-	1	-
63C	129+86.85	28.90' LT	844.39	838.81	4.58	-	-	-	-	-	-	-	-	-	1	-
63D	129+80.20	45.22' LT	844.69	838.63	5.06	-	-	-	-	-	-	-	-	-	1	-
64	129+74.29	16.50' LT	844.66	836.38	7.03	1	-	-	-	-	-	-	1	-	1	POSITION STAIRS/CASTING OUT OF BIKE LANE.
70	132+46.89	16.50' LT	843.62	835.57	6.80	1	-	-	-	-	-	-	1	-	1	POSITION STAIRS/CASTING OUT OF BIKE LANE.
70A	132+82.71	45.82' LT	843.39	837.53	4.86	-	-	-	-	-	-	-	-	-	1	-
70B	132+49.18	43.87' LT	843.57	837.36	5.21	-	-	-	-	-	-	-	-	-	1	-
70C	132+40.00	26.91' LT	843.38	837.17	5.21	-	-	-	-	-	-	-	-	-	1	-
70D	132+50.00	17.50' RT	843.54	837.38	5.16	-	-	-	-	-	-	-	-	-	1	-
71	134+16.00	16.50' LT	842.83	835.07	6.51	1	-	-	-	-	-	-	-	-	1	POSITION STAIRS/CASTING OUT OF BIKE LANE.
71A	133+78.68	23.50' LT	842.76	836.85	4.91	-	-	-	-	-	-	-	-	-	1	-
71B	134+16.00	23.50' LT	842.69	836.48	5.21	-	-	-	-	-	-	-	-	-	1	-
71C	134+02.53	17.50' RT	842.85	836.82	5.03	-	-	-	-	-	-	-	-	-	1	-
71D	134+16.00	17.50' RT	842.86	836.72	5.14	-	-	-	-	-	-	-	-	-	1	-
72	135+72.73	18.22' LT	842.91	834.61	7.05	1	-	-	-	-	-	-	-	-	1	POSITION STAIRS/CASTING OUT OF BIKE LANE.
72A	135+80.97	17.50' RT	842.93	834.79	7.14	-	-	-	-	-	-	-	-	-	1	-
72B	136+15.00	17.50' RT	842.80	836.96	4.84	-	-	-	-	-	-	-	-	-	1	-
73	136+11.11	16.50' LT	842.87	837.95	3.67	1	-	-	1	-	-	-	-	-	1	POSITION STAIRS/CASTING OUT OF BIKE LANE.
SUBTOTAL						13	1	1	2	-	-	4	4	-	45	

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STORM SEWER

STRUCTURE	STATION	OFFSET*	RIM**	INVERT***	DEPTH****	611.0530	611.0545	611.2003	611.2004	611.2005	611.2006	611.2008	611.2010	611.8110	650.4000	REMARKS
			ELEVATION	ELEVATION	FT	MANHOLE COVERS EACH	MANHOLE COVERS EACH	MANHOLES 3-FT DIAMETER EACH	MANHOLES 4-FT DIAMETER EACH	MANHOLES 5-FT DIAMETER EACH	MANHOLES 6-FT DIAMETER EACH	MANHOLES 8-FT DIAMETER EACH	MANHOLES 10-FT DIAMETER EACH	ADJUSTING MANHOLE COVERS EACH	CONSTRUCTION STAKING STORM SEWER EACH	
73A	136+11.11	23.50'LT	842.76	837.00	4.76	-	-	-	-	-	-	-	-	-	1	-
74	138+84.30	16.50'LT	844.29	840.00	3.04	1	-	-	-	-	1	-	-	-	1	POSITION STAIRS/CASTING OUT OF BIKE LANE.
74A	139+05.10	17.50'RT	844.34	839.30	4.04	-	-	-	-	-	-	-	-	-	1	-
75	141+88.45	16.50'LT	846.49	842.29	2.95	1	-	-	-	-	1	-	-	-	1	POSITION STAIRS/CASTING OUT OF BIKE LANE.
75A	142+10.00	17.50'RT	846.67	841.49	4.18	-	-	-	-	-	-	-	-	-	1	-
76	144+97.94	16.50'LT	849.49	845.11	3.13	1	-	-	1	-	-	-	-	-	1	POSITION STAIRS/CASTING OUT OF BIKE LANE.
76A	144+97.94	17.50'RT	849.42	843.78	4.64	-	-	-	-	-	-	-	-	-	1	-
77	148+06.75	16.50'LT	854.46	848.45	4.76	1	-	-	1	-	-	-	-	-	1	POSITION STAIRS/CASTING OUT OF BIKE LANE.
78	148+06.94	33.80'LT	854.39	849.87	3.27	1	-	1	-	-	-	-	-	-	1	-
78A	147+92.37	40.99'LT	854.23	848.03	5.20	-	-	-	-	-	-	-	-	-	1	-
78B	148+21.49	40.98'LT	854.21	848.03	5.18	-	-	-	-	-	-	-	-	-	1	-
79	148+06.15	39.92'RT	855.06	850.42	3.39	1	-	1	-	-	-	-	-	-	1	-
79A	147+86.77	39.81'RT	854.43	848.95	4.48	-	-	-	-	-	-	-	-	-	1	-
79B	148+25.68	40.12'RT	854.73	848.95	4.78	-	-	-	-	-	-	-	-	-	1	-
90	150+91.66	16.50'LT	852.80	847.80	3.75	1	-	-	-	1	-	-	-	-	1	POSITION STAIRS/CASTING OUT OF BIKE LANE.
90A	151+12.00	41.62'LT	852.27	846.45	4.82	-	-	-	-	-	-	-	-	-	1	-
90B	150+91.66	23.50'LT	852.69	846.12	5.57	-	-	-	-	-	-	-	-	-	1	-
90C	150+91.66	17.50'RT	852.78	846.57	5.21	-	-	-	-	-	-	-	-	-	1	-
91	154+08.36	16.50'LT	847.75	841.14	5.36	1	-	-	1	-	-	-	-	-	1	POSITION STAIRS/CASTING OUT OF BIKE LANE.
91A	154+08.36	23.50'LT	847.64	841.46	5.18	-	-	-	-	-	-	-	-	-	1	-
91B	154+08.36	17.50'RT	847.60	841.73	4.87	-	-	-	-	-	-	-	-	-	1	-
92	157+34.66	16.50'LT	826.13	821.56	3.32	1	-	-	1	-	-	-	-	-	1	POSITION STAIRS/CASTING OUT OF BIKE LANE.
92A	157+34.66	23.50'LT	826.02	819.88	5.14	-	-	-	-	-	-	-	-	-	1	-
92B	157+31.85	17.50'RT	826.15	821.48	3.67	-	-	-	-	-	-	-	-	-	1	-
93	159+60.01	16.50'LT	819.65	809.44	8.96	1	-	-	-	-	1	-	-	-	1	POSITION STAIRS/CASTING OUT OF BIKE LANE.
93A	159+17.06	23.50'LT	819.74	813.58	5.16	-	-	-	-	-	-	-	-	-	1	-
93B	159+37.47	42.05'LT	819.36	813.05	5.31	-	-	-	-	-	-	-	-	-	1	-
93C	159+67.31	42.05'LT	819.38	812.92	5.46	-	-	-	-	-	-	-	-	-	1	-
93D	159+78.23	25.84'LT	819.56	813.12	5.44	-	-	-	-	-	-	-	-	-	1	-
93E	159+44.60	17.50'RT	819.55	814.40	4.15	-	-	-	-	-	-	-	-	-	1	-
93F	159+54.60	17.50'RT	819.50	814.30	4.20	-	-	-	-	-	-	-	-	-	1	-
93G	159+67.17	52.05'LT	819.43	813.27	5.16	-	-	-	-	-	-	-	-	-	1	-
93H	159+37.53	52.05'LT	819.42	813.41	5.01	-	-	-	-	-	-	-	-	-	1	-
94	163+65.00	16.50'LT	818.98	806.40	11.33	1	-	-	-	-	1	-	-	-	1	POSITION STAIRS/CASTING OUT OF BIKE LANE.
94A	163+75.00	17.50'RT	818.79	812.78	5.01	-	-	-	-	-	-	-	-	-	1	-
94B	163+65.00	17.50'RT	818.83	812.43	5.40	-	-	-	-	-	-	-	-	-	1	-
95	163+92.57	23.50'LT	818.78	804.12	13.66	-	-	-	-	-	-	-	-	-	1	-
95A	164+00.00	23.50'LT	818.78	812.47	5.31	-	-	-	-	-	-	-	-	-	1	-
96	164+34.22	71.32'LT	809.34	801.10	6.99	1	-	-	-	1	-	-	-	-	1	-
97	163+63.49	28.91'RT	816.63	811.03	4.35	1	-	-	-	1	-	-	-	-	1	-
98	163+92.19	11.00'RT	818.97	810.00	7.72	1	-	1	-	-	-	-	-	-	1	-
99	164+45.65	71.32'LT	809.25	804.27	3.73	1	-	-	1	-	-	-	-	-	1	-
100	154+53.23	45.38'LT	846.42	-	-	-	-	-	-	-	-	-	-	1	1	ADJUSTING MANHOLE COVERS.
100A	154+31.05	41.57'LT	846.43	-	-	-	-	-	-	-	-	-	-	-	1	ADJUSTING CATCH BASIN COVERS.
100B	154+62.15	42.19'LT	846.04	-	-	-	-	-	-	-	-	-	-	-	1	ADJUSTING CATCH BASIN COVERS.
SUBTOTAL						15	-	3	5	3	4	-	-	1	45	

REMARKS:
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 **RIM ELEV IS AT THE INLET COVER FLANGE LOCATION
 ***FOR STRUCTURES WITH SUMPS, THE INVERT ELEVATION IS THE ELEVATION OF THE SUMP. FOR STRUCTURES WITHOUT SUMPS, THE INVERT ELEVATION IS THE ELEVATION OF THE LOWEST PIPE FLOW LINE
 ****DEPTH = RIM ELEV - TOP OF STRUCTURE BASE ELEV - COVER HEIGHT - ADJUSTMENT
 ADJUSTMENT HEIGHT IS 6 INCHES UNLESS OTHERWISE NOTED

STORM SEWER

STRUCTURE	STATION	OFFSET*	RIM** ELEVATION	INVERT*** ELEVATION	DEPTH**** FT	611.0530	611.0545	611.2003	611.2004	611.2005	611.2006	611.2008	611.2010	611.8110	650.4000	REMARKS
						MANHOLE COVERS TYPE J EACH	MANHOLE COVERS TYPE L EACH	MANHOLES 3-FT DIAMETER EACH	MANHOLES 4-FT DIAMETER EACH	MANHOLES 5-FT DIAMETER EACH	MANHOLES 6-FT DIAMETER EACH	MANHOLES 8-FT DIAMETER EACH	MANHOLES 10-FT DIAMETER EACH	ADJUSTING MANHOLE COVERS EACH	CONSTRUCTION STAKING STORM SEWER EACH	
110	169+50.00	8.50'LT	816.48	809.86	5.37	1	-	-	1	-	-	-	-	-	1	POSITION STAIRS/CASTING OUT OF TIRE PATH.
110A	169+50.00	17.50'LT	816.33	810.41	4.92	-	-	-	-	-	-	-	-	-	1	-
110B	169+50.00	17.50'RT	816.97	810.58	5.39	-	-	-	-	-	-	-	-	-	1	-
111	172+64.49	8.50'LT	810.28	806.71	2.32	1	-	-	1	-	-	-	-	-	1	POSITION STAIRS/CASTING OUT OF TIRE PATH.
111A	172+81.12	20.06'LT	809.68	805.03	3.73	-	-	-	-	-	-	-	-	-	1	5 INCHES OF ADJUSTMENT.
112	172+81.57	8.66'RT	810.00	806.59	2.16	1	-	-	1	-	-	-	-	-	1	-
112A	172+78.62	17.50'RT	810.05	804.87	4.18	-	-	-	-	-	-	-	-	-	1	-
201	157+67.22	40.76'RT	823.71	816.38	6.08	1	-	-	-	1	-	-	-	-	1	-
201A	157+53.63	50.75'RT	823.71	817.09	5.62	-	-	-	-	-	-	-	-	-	1	-
201B	157+53.81	40.76'RT	823.61	816.99	5.62	-	-	-	-	-	-	-	-	-	1	-
201C	157+88.54	50.76'RT	823.62	817.24	5.38	-	-	-	-	-	-	-	-	-	1	-
201D	157+88.45	40.76'RT	823.42	817.14	5.28	-	-	-	-	-	-	-	-	-	1	-
202	157+60.91	16.50'LT	824.68	812.25	11.18	1	-	-	-	-	1	-	-	-	1	POSITION STAIRS/CASTING OUT OF BIKE LANE.
274	138+84.44	41.65'LT	844.37	841.00	2.12	1	-	-	1	-	-	-	-	-	1	-
274A	138+63.42	47.01'LT	844.20	839.41	3.79	-	-	-	-	-	-	-	-	-	1	-
274B	138+99.46	28.75'LT	843.98	839.44	3.71	-	-	-	-	-	-	-	-	-	1	4 INCHES OF ADJUSTMENT
274C	138+92.71	46.06'LT	844.38	839.09	4.29	-	-	-	-	-	-	-	-	-	1	-
275	141+88.45	38.82'LT	846.77	842.76	2.76	1	-	1	-	-	-	-	-	-	1	-
275A	141+67.47	43.29'LT	846.68	841.51	4.17	-	-	-	-	-	-	-	-	-	1	-
275B	142+09.45	26.35'LT	846.30	841.55	3.75	-	-	-	-	-	-	-	-	-	1	-
275C	141+96.82	43.65'LT	846.83	841.45	4.38	-	-	-	-	-	-	-	-	-	1	-
276	144+97.94	39.13'LT	849.60	845.13	3.22	1	-	1	-	-	-	-	-	-	1	-
276A	144+76.67	41.03'LT	849.26	843.83	4.43	-	-	-	-	-	-	-	-	-	1	-
276B	145+16.73	26.09'LT	849.59	843.86	4.73	-	-	-	-	-	-	-	-	-	1	-
276C	145+06.38	41.39'LT	849.67	843.77	4.90	-	-	-	-	-	-	-	-	-	1	-
293	159+58.91	42.05'LT	819.51	814.84	3.42	1	-	-	1	-	-	-	-	-	1	-
EX-8	135+72.63	52.61'LT	842.93	-	-	-	-	-	-	-	-	-	-	1	1	ADJUSTING MANHOLE COVERS.
EX-8A	135+50.33	49.33'LT	842.66	-	-	-	-	-	-	-	-	-	-	-	1	ADJUSTING CATCH BASIN COVERS.
EX-8B	135+83.16	49.84'LT	842.62	-	-	-	-	-	-	-	-	-	-	-	1	ADJUSTING INLET COVERS.
EX-72	135+79.97	32.48'RT	842.64	-	-	-	-	-	-	-	-	-	-	-	1	ADJUSTING INLET COVERS.
SUBTOTAL						9	-	2	5	1	1	-	-	1	30	
TOTAL 0010						54	1	7	17	7	6	6	8	2	165	

REMARKS:

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ADJUSTMENT HEIGHT IS 6 INCHES UNLESS OTHERWISE NOTED

STORM SEWER

STRUCTURE	STATION	OFFSET*	RIM** ELEVATION	INVERT*** ELEVATION	DEPTH**** FT	SPV.0060.05	SPV.0060.06	SPV.0060.07	REMARKS
						MANHOLE 6x3- FT EACH	MANHOLE 7.5x3-FT EACH	MANHOLE 9.5x7.5-FT EACH	
41	118+00.00	17.50' LT	843.62	838.49	3.96	1	-	-	5 INCHES OF ADJUSTMENT.
60	125+99.92	16.50' LT	842.99	837.01	4.81	-	1	-	POSITION STAIRS/CASTING OUT OF BIKE LANE. 5 INCHES OF ADJUSTMENT.
71	134+16.00	16.50' LT	842.83	835.07	6.51	-	1	-	POSITION STAIRS/CASTING OUT OF BIKE LANE.
72	135+72.73	18.22' LT	842.91	834.61	7.05	-	-	1	POSITION STAIRS/CASTING OUT OF BIKE LANE.
TOTAL 0010						1	2	1	

REMARKS:

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ADJUSTMENT HEIGHT IS 6 INCHES UNLESS OTHERWISE NOTED

WATER & SANITARY

CATEGORY	STATION	TO	STATION	LOCATION	612.0902.S	SPV.0060.02	SPV.0060.03	SPV.0060.04	
					INSULATION BOARD POLYSTYRENE 1-INCH SY	ADJUST WATER VALVE BOX EACH	ADJUST CURB STOP EACH	ADJUSTING SANITARY SEWER MANHOLE EACH	
0030	99+50	-	105+00			6	1	-	
	105+00	-	110+00			3	3	5	
	110+00	-	115+00			5	3	5	
	115+00	-	120+00			5	1	4	
	120+00	-	125+00			6	-	4	
	125+00	-	130+00			2	6	-	
	130+00	-	135+00			2	6	-	
	135+00	-	140+00				6	-	
	140+00	-	145+50				7	-	
	145+50	-	150+00				4	-	
	150+00	-	155+00				4	3	
	155+00	-	160+00			2	5	4	
	160+00	-	165+00				3	4	
	165+00	-	170+00				4	2	
	170+00	-	174+00				2	1	
	TOTAL 0030					6	72	22	43

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EROSION CONTROL

CATEGORY	STATION	TO	STATION	#	606.0300	625.0100	625.0500	628.1504	628.1905	628.1910	628.2008	628.7005	628.7010	628.7015	628.7020	628.7504	628.7560	628.7570	#	SPV.0060.09
				RIPRAP HEAVY	TOPSOIL	SALVAGED TOPSOIL	SILT FENCE	MOBILIZATIONS EROSION CONTROL	MOBILIZATIONS EMERGENCY EROSION CONTROL	EROSION MAT URBAN CLASS I TYPE B	INLET PROTECTION TYPE A	INLET PROTECTION TYPE B	INLET PROTECTION TYPE C	INLET PROTECTION TYPE D	TEMPORARY DITCH CHECKS	TRACKING PADS	ROCK BAGS	GEOTEXTILE TYPE HR	TEMPORARY WATER DIVERSION	
				CY	SY	SY	LF	EACH	EACH	SY	EACH	EACH	EACH	EACH	EACH	LF	EACH	EACH	SY	EACH
0010	99+50	-	105+00	-	1,327	-	222	-	-	-	8	-	12	4	-	-	-	-	-	4
	105+00	-	110+00	-	794	588	-	-	-	682	7	-	-	13	16	-	-	-	-	-
	110+00	-	115+00	-	1,108	186	-	-	-	186	4	-	6	-	-	-	-	-	-	-
	115+00	-	120+00	-	1,194	62	81	-	-	-	9	2	6	8	-	-	-	-	-	-
	120+00	-	125+00	-	829	280	501	-	-	-	12	-	9	14	-	-	-	17	-	-
	125+00	-	130+00	-	784	72	148	-	-	-	10	-	5	8	-	-	-	-	-	-
	130+00	-	135+00	-	931	-	-	-	-	-	8	-	4	5	-	-	-	-	-	-
	135+00	-	140+00	-	1,004	-	-	-	-	-	7	-	4	7	-	-	-	-	-	-
	140+00	-	145+50	-	904	17	-	-	-	-	8	-	6	2	-	-	-	-	-	-
	145+50	-	150+00	-	645	49	-	-	-	-	4	-	4	-	-	-	-	-	-	-
	150+00	-	155+00	-	812	-	-	-	-	-	7	-	4	5	-	-	-	-	-	-
	155+00	-	160+00	-	731	2	7	-	-	-	14	-	15	8	-	-	-	-	-	-
	160+00	-	165+00	42	875	174	647	-	-	366	4	-	-	6	-	-	-	17	63	-
	165+00	-	170+00	-	915	-	292	-	-	-	2	-	2	-	-	-	-	-	-	-
	170+00	-	174+00	-	641	-	318	-	-	-	2	-	8	-	-	-	-	17	-	-
UNDISTRIBUTED				-	-	-	-	5	5	-	-	-	-	-	-	-	8	-	-	-
TOTAL 0010				42	13,494	1,430	2,216	5	5	1,234	106	2	85	80	16	8	51	63	4	

#ADDITIONAL QUANTITIES LISTED ON STRUCTURE B-67-401 QUANTITIES

LANDSCAPING

CATEGORY	STATION	TO	STATION	LOCATION	627.0200	628.6510	629.0210	630.0140	630.0200	630.0500	631.0300	631.1000
					MULCHING	SOIL STABILIZER TYPE B	FERTILIZER TYPE B	SEEDING MIXTURE NO. 40	SEEDING TEMPORARY	SEED WATER	SOD WATER	SOD LAWN
					SY	ACRE	CWT	LB	LB	MGAL	MGAL	SY
0010	99+50	-	105+00		-	-	1	-	-	-	29.8	1,327
	105+00	-	110+00		590	-	1	27	-	13.2	17.9	794
	110+00	-	115+00		190	-	1	9	-	4.2	24.9	1,108
	115+00	-	120+00		70	-	1	3	-	1.4	26.9	1,194
	120+00	-	125+00		280	-	1	13	-	6.3	18.7	829
	125+00	-	130+00		80	-	1	4	-	1.7	17.6	784
	130+00	-	135+00		-	-	1	-	-	-	20.9	931
	135+00	-	140+00		-	-	1	-	-	-	22.6	1,004
	140+00	-	145+50		20	-	1	1	-	0.4	20.3	904
	145+50	-	150+00		50	-	1	3	-	1.1	14.5	645
	150+00	-	155+00		-	-	1	-	-	-	18.3	812
	155+00	-	160+00		10	-	1	1	-	0.1	16.4	731
	160+00	-	165+00		180	-	1	8	-	3.9	19.7	875
	165+00	-	170+00		-	-	1	-	-	-	20.6	915
	170+00	-	174+00		-	-	1	-	-	-	14.4	641
UNDISTRIBUTED					13,750	0.3	1	7	468	392.2	31	1,349
TOTAL 0010					15,220	0.3	16	76	468	424.5	334.5	14,843

TRAFFIC CONTROL

CATEGORY	LOCATION	#				#				#			#		+ ESTIMATED CALENDAR				
		643.0300		643.0420		643.0410		643.0705		643.0715		643.0900		643.1070		603.8000	603.8125		
		TRAFFIC CONTROL DRUMS		TRAFFIC BARRICADES TYPE III		TRAFFIC BARRICADES TYPE II		TRAFFIC WARNING LIGHTS TYPE A		TRAFFIC WARNING LIGHTS TYPE C		TRAFFIC CONTROL SIGNS		TRAFFIC COVERING CONTROL CONES 42-INCH		CONCRETE BARRIER TEMPORARY PRECAST DELIVERED LF	CONCRETE BARRIER TEMPORARY PRECAST INSTALLED LF		
		+ EACH	DAY	+ EACH	DAY	+ EACH	DAY	+ EACH	DAY	+ EACH	DAY	+ EACH	DAYS	+ EACH	+ EACH	DAY	LF	LF	
0010	1A	313	23,006	82	6,027	-	-	166	12,201	20	1,470	145	10,658	-	27	1,985	238	238	74
	1B	305	9,608	74	2,331	-	-	150	4,725	6	189	150	4,725	-	27	851	338	338	32
	2	170	13,090	66	5,082	-	-	134	10,318	-	-	112	8,624	-	-	-	238	238	77
	3	306	9,792	82	2,624	-	-	166	5,312	6	192	132	4,224	-	-	-	-	-	32
UNDISTRIBUTED		-	-	-	-	10	1,000	-	-	-	-	-	-	5	-	-	-	-	-
TOTAL 0010		55,495		16,064		1,000		32,556		1,851		28,231		5		2,835	814	814	

ADDITIONAL QUANTITIES SHOWN ELSEWHERE. SEE "DETOUR", "PEDESTRIAN DETOUR", AND "TEMPORARY PEDESTRIAN ACCOMMODATIONS" TABLE.
 ## ADDITIONAL QUANTITIES SHOWN ELSEWHERE. SEE "TEMPORARY PEDESTRIAN ACCOMMODATIONS" TABLE.
 + FOR INFORMATION PURPOSES ONLY.

DETOUR

CATEGORY	LOCATION	#		+		ESTIMATED CALENDAR DAYS
		643.0900	643.1050	TRAFFIC CONTROL SIGNS	TRAFFIC CONTROL SIGNS PCMS	
		+ EACH	DAY	+ EACH	DAY	DAYS
0010	MENOMONEE AVE	210	44,940	2	14	214
TOTAL 0010			44,940		14	

ADDITIONAL QUANTITIES SHOWN ELSEWHERE. SEE "TRAFFIC CONTROL", "TEMPORARY PEDESTRIAN DETOUR", AND "TEMPORARY PEDESTRIAN ACCOMMODATIONS" TABLE.
 + FOR INFORMATIONAL PURPOSES ONLY.

PEDESTRIAN DETOUR

CATEGORY	STAGE	#		ESTIMATED CALENDAR DAYS
		643.0900	TRAFFIC CONTROL SIGNS	
		+ EACH	DAY	DAYS
0010	1A	12	882	74
TOTAL 0010			882	

ADDITIONAL QUANTITIES SHOWN ELSEWHERE. SEE "TRAFFIC CONTROL", "DETOUR", AND "TEMPORARY PEDESTRIAN ACCOMMODATIONS" TABLE.
 + FOR INFORMATIONAL PURPOSES ONLY.

TEMPORARY PAVEMENT MARKING

CATEGORY	STAGE	643.3105		643.3150		###	
		TEMPORARY MARKING LINE PAINT 4-INCH (YELLOW)	TEMPORARY MARKING LINE PAINT 4-INCH (WHITE)	TEMPORARY MARKING LINE REMOVABLE TAPE 4-INCH (YELLOW)	TEMPORARY MARKING LINE REMOVABLE TAPE 4-INCH (WHITE)	643.3165 TEMPORARY MARKING LINE PAINT 6-INCH (WHITE)	643.3850 TEMPORARY MARKING STOP LINE REMOVABLE TAPE 18-INCH (WHITE)
		LF	LF	LF	LF	LF	LF
0010	1A	738	5,379	186	229	-	20
	1B	231	882	-	-	111	9
	2	-	-	7,168	201	-	27
	3	-	-	174	842	-	20
SUBTOTAL		969	6,261	7,528	1,272	111	76
TOTAL 0010		7,230		8,800		111	76

ADDITIONAL QUANTITIES SHOWN ELSEWHERE. SEE "TEMPORARY PEDESTRIAN ACCOMMODATIONS" TABLE.
 + FOR INFORMATIONAL PURPOSES ONLY.

TEMPORARY PEDESTRIAN ACCOMMODATIONS

CATEGORY	STAGE	# 643.0300		## 643.0900		### 643.3165		643.3180	644.1430	644.1440	644.1601	644.1605	644.1810	+ ESTIMATED CALENDAR DAYS
		+EACH	DAY	+EACH	DAY	PAINT 6-INCH (WHITE) LF	REMOVABLE TAPE 6-INCH (WHITE) LF	TEMPORARY PEDESTRIAN SURFACE PLATE SF	TEMPORARY PEDESTRIAN SURFACE MATTING SF	TEMPORARY PEDESTRIAN CURB RAMP +EACH DAY	TEMPORARY DETECTABLE WARNING FIELD SF	TEMPORARY PEDESTRIAN BARRICADE LF		
0010	1A	245	18,008	70	5,145	245	-	490	-	36	2,646	150	1,083	74
	1B	-	-	-	-	-	-	-	1,570	-	-	40	92	32
	2	-	-	-	-	-	-	-	1,760	-	-	20	-	77
	3	156	4,992	-	-	-	286	900	150	13	416	130	370	32
TOTAL 0010		18,008	5,145	245	5,145	245	286	1,390	3,480	3,062	340	1,545		

ADDITIONAL QUANTITIES SHOWN ELSEWHERE. SEE "TRAFFIC CONTROL" TABLE.
 ## ADDITIONAL QUANTITIES SHOWN ELSEWHERE. SEE "TRAFFIC CONTROL", "DETOUR", AND "PEDESTRIAN DETOUR" TABLE.
 ### ADDITIONAL QUANTITIES SHOWN ELSEWHERE. SEE "TEMPORARY PAVEMENT MARKING" TABLE.
 + FOR INFORMATION PURPOSES ONLY.

PAVEMENT MARKING

CATEGORY	STATION TO	STATION	646.1020 MARKING LINE EPOXY 4-INCH				646.3020 MARKING LINE EPOXY			646.5020 MARKING ARROW EPOXY			646.5120 MARKING WORD EPOXY		646.5220 MARKING SYMBOL EPOXY		646.6120	646.7120	646.7420	646.7520
			YELLOW LF	37.5-FOOT LINE LF	WHITE LF	WHITE SKIP 3-FOOT LINE 9-FOOT GAP LF	8-INCH (WHITE) LF	PAVEMENT MARKING ARROWS EPOXY TYPE 2 (WHITE) EACH	PAVEMENT MARKING ARROWS EPOXY TYPE 3 (WHITE) EACH	PAVEMENT MARKING ARROWS BIKE LANE EPOXY EACH	PAVEMENT MARKING WORDS BIKE LANE ENDS EPOXY (WHITE) EACH	PAVEMENT MARKING SYMBOLS BIKE LANE EPOXY (WHITE) EACH	MARKING STOP LINE EPOXY 18-INCH (WHITE) LF	MARKING DIAGONAL EPOXY 12-INCH (YELLOW) LF	MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE) LF	MARKING CROSSWALK EPOXY BLOCK STYLE 24-INCH (WHITE) LF				
0010	99+50	- 105+00	1,219	-	871	13	150	2	2	4	-	4	4	44	59	81	-			
	105+00	- 110+00	335	65	1,248	25	-	-	-	4	-	4	4	17	-	72	-			
	110+00	- 115+00	-	103	1,247	25	-	-	-	4	-	4	4	15	-	72	-			
	115+00	- 120+00	-	106	1,350	-	-	-	-	4	-	4	4	17	-	69	70			
	120+00	- 125+00	-	87	996	50	-	-	-	3	-	3	3	33	-	142	-			
	125+00	- 130+00	-	85	1,077	25	-	-	-	4	-	4	4	34	-	165	-			
	130+00	- 135+00	-	107	1,150	50	-	-	-	4	-	3	3	16	-	71	-			
	135+00	- 140+00	-	89	1,013	50	-	-	-	3	-	4	4	28	-	125	-			
	140+00	- 145+50	24	102	1,202	37	-	-	-	4	-	4	4	28	-	134	-			
	145+50	- 150+00	526	55	1,061	14	-	-	-	3	-	3	3	32	-	144	132			
	150+00	- 155+00	717	-	1,082	34	-	-	-	4	-	4	4	28	-	132	-			
	155+00	- 160+00	708	-	1,144	35	-	-	-	4	-	4	4	30	-	146	-			
	160+00	- 165+00	1,000	-	1,425	76	-	-	-	4	-	4	4	-	-	-	-			
	165+00	- 170+00	1,000	-	1,027	-	-	-	-	5	-	4	4	-	-	-	-			
	170+00	- 174+00	651	-	475	15	-	-	-	2	3	3	3	-	-	-	-			
SUBTOTAL			6,180	799	16,368	449	150	2	2	56	3	56	322	59	1,353	202				
TOTAL 0010			23,796				150		60		3	56	322	59	1,353	202				

3

PAVEMENT MARKING

CATEGORY	STATION TO STATION	646.6464	646.6468
		4-INCH EPOXY MARKING	8-INCH EPOXY MARKING
0010	UNDISTRIBUTED	4,759	150
TOTAL 0010		4,759	150

CONSTRUCTION STAKING

CATEGORY	STATION TO STATION	LOCATION	650.4500	650.5000	650.5500	650.9000	650.9500.01	650.9911.01
			CONSTRUCTION STAKING SUBGRADE LF	CONSTRUCTION STAKING BASE LF	CONSTRUCTION STAKING CURB GUTTER & GUTTER LF	CONSTRUCTION STAKING CURB RAMPS EACH	CONSTRUCTION STAKING SIDEWALK (2720-07-71) EACH	CONSTRUCTION STAKING SUPPLEMENTAL CONTROL (2720-07-71) EACH
0010	99+50 - 105+00		500	500	1,038	2	-	-
	105+00 - 110+00		500	500	1,024	2	-	-
	110+00 - 115+00		500	500	1,045	2	-	-
	115+00 - 120+00		500	500	1,046	4	-	-
	120+00 - 125+00		500	500	1,011	4	-	-
	125+00 - 130+00		500	500	1,008	4	-	-
	130+00 - 135+00		500	500	1,038	2	-	-
	135+00 - 140+00		500	500	1,068	4	-	-
	140+00 - 145+50		550	550	1,153	6	-	-
	145+50 - 150+00		450	450	910	8	-	-
	150+00 - 155+00		500	500	1,014	4	-	-
	155+00 - 160+00		500	500	1,043	4	-	-
	160+00 - 165+00		500	500	1,001	-	-	-
	165+00 - 170+00		500	500	1,001	-	-	-
	170+00 - 174+00		400	400	730	-	-	-
UNDISTRIBUTED			-	-	-	-	1	1
TOTAL 0010			7,400	7,400	15,130	46	1	1

3

SAWING

CATEGORY	STATION TO STATION	LOCATION	690.0150	690.0250
			SAWING ASPHALT LF	SAWING CONCRETE LF
0010	99+50 - 105+00		202	77
	105+00 - 110+00		134	-
	110+00 - 115+00		133	22
	115+00 - 120+00		110	47
	120+00 - 125+00		77	29
	125+00 - 130+00		131	102
	130+00 - 135+00		107	56
	135+00 - 140+00		164	80
	140+00 - 145+50		137	94
	145+50 - 150+00		78	116
	150+00 - 155+00		87	129
	155+00 - 160+00		165	78
	160+00 - 165+00		-	87
	165+00 - 170+00		137	36
	170+00 - 174+00		162	15
TOTAL 0010			1,824	968

UTILITY LINE OPENING

CATEGORY	LOCATION	SPV.0060.01 UTILITY LINE OPENING EACH
0010	PROJECT-WIDE	20
TOTAL 0010		20

MISCELLANEOUS

CATEGORY	STATION TO STATION	LOCATION	#	513.2001	619.1000	643.5000	648.0100	SPV.0060.12	SPV.0075.01	SPV.0090.13
			RAILING PIPE LF	MOBILIZATION EACH	TRAFFIC CONTROL EACH	LOCATING NO-PASSING ZONES MI	SECTION CORNER MONUMENTS EACH	STREET SWEEPING HRS	FIELD OFFICE TYPE T EACH	
0010	160+00 - 165+00		383	-	-	-	-	-	-	-
	126+47 - 126+47	SHADY LN	-	-	-	-	1	-	-	-
	152+87 - 152+87		-	-	-	-	1	-	-	-
UNDISTRIBUTED			-	1	1	1.38	-	50	1	
TOTAL 0010			383	1	1	1.38	2	50	1	

ADDITIONAL QUANTITIES SHOWN ELSEWHERE. SEE STRUCTURE B-67-401 PLANS.

PERMANENT SIGNING

SIGN #	SIGN CODE	SHEET #	SIGN SIZE	W IN.	X IN.	H IN.	637.2210		637.2230		634.0808		634.0811		634.0814		SIGN MOUNTED ON SAME POST AS #	DESCRIPTION	NOTES
							SIGNS TYPE II REFLECTIVE SF	SIGNS TYPE II REFLECTIVE F SF	POSTS TUBULAR STEEL 2X2-INCH EACH	POSTS TUBULAR STEEL 2X2-INCH EACH	POSTS TUBULAR STEEL 2X2-INCH EACH	SIGN							
1-1	R1-1	1	OF 15 2S 30 X 30	30	X	30	6.25	-	-	-	1	-	-	-	-	STOP	-	-	
1-2	R1-1	1	OF 15 2S 30 X 30	30	X	30	6.25	-	-	-	-	-	-	-	1-1	STOP	-	-	
1-3	R1-3P	1	OF 15 1 18 X 6	18	X	6	0.75	-	-	-	-	-	-	-	1-1	ALL WAY	-	-	
1-4	D9-2	1	OF 15 2S 24 X 24	24	X	24	4.00	-	-	-	1	-	-	-	-	HOSPITAL	-	-	
1-5	M6-1	1	OF 15 2S 21 X 21	21	X	21	3.06	-	-	-	-	-	-	-	1-4	ARROW - RIGHT, LEFT, OR AHEAD	-	-	
1-6	S1-1	1	OF 15 2S 36 X 36	36	X	36	-	9.00	-	-	-	-	1	-	-	SCHOOL ADVANCE	-	-	
1-7	S16-9P	1	OF 15 1 24 X 12	24	X	12	-	2.00	-	-	-	-	-	-	1-6	AHEAD PLAQUE (FLUORESCENT YELLOW GREEN)	-	-	
1-8	R5-2	1	OF 15 2S 24 X 24	24	X	24	4.00	-	-	-	-	-	1	-	-	NO TRUCKS SYMBOL	-	-	
1-9	R12-1	1	OF 15 2S 24 X 30	24	X	30	5.00	-	-	-	-	-	-	-	1-8	WEIGHT LIMIT 3 TONS	-	-	
1-10	D3-1	1	OF 15 2S 36 X 12	36	X	12	-	-	-	-	-	-	-	-	1-12	MENOMONEE AVE	BY OTHERS	-	
1-11	D3-1	1	OF 15 2S 36 X 12	36	X	12	-	-	-	-	-	-	-	-	1-12	SHADY LA	BY OTHERS	-	
1-12	R1-1	1	OF 15 2S 30 X 30	30	X	30	6.25	-	-	-	-	-	1	-	-	STOP	-	-	
1-13	R9-3A	1	OF 15 2S 24 X 24	24	X	24	4.00	-	-	1	-	-	-	-	-	NO PEDESTRIAN CROSSING (SYMBOL SIGN)	-	-	
1-14	R9-3BL	1	OF 15 2S 18 X 12	18	X	12	1.50	-	-	-	-	-	-	-	1-13	USE CROSSWALK (LEFT ARROW)	-	-	
2-1	R3-8LTR	2	OF 15 2S 54 X 30	54	X	30	11.25	-	-	-	-	-	-	-	-	LEFT ONLY, THROUGH-RIGHT	-	-	
2-2	W3-1	2	OF 15 2S 36 X 36	36	X	36	-	9.00	-	-	-	-	1	-	-	STOP AHEAD	-	-	
2-3	R1-1	2	OF 15 2S 30 X 30	30	X	30	6.25	-	-	-	-	-	1	-	-	STOP	-	-	
2-4	D3-1	2	OF 15 2S 36 X 12	36	X	12	-	-	-	-	-	-	-	-	2-3	MENOMONEE AVE	BY OTHERS	-	
2-5	D3-1	2	OF 15 2S 36 X 12	36	X	12	-	-	-	-	-	-	-	-	2-3	LYNWOOD DR	BY OTHERS	-	
2-6	R2-1	2	OF 15 2S 24 X 30	24	X	30	5.00	-	-	-	1	-	-	-	-	SPEED LIMIT 30 MPH	-	-	
3-1	R3-1	3	OF 15 2S 24 X 24	24	X	24	4.00	-	-	-	1	-	-	-	-	NO RIGHT TURN SYMBOL	-	-	
3-2	R1-1	3	OF 15 2S 30 X 30	30	X	30	6.25	-	-	-	1	-	-	-	-	STOP	-	-	
3-3	D3-1	3	OF 15 2S 36 X 12	36	X	12	-	-	-	-	-	-	-	-	3-2	MENOMONEE AVE	BY OTHERS	-	
3-4	D3-1	3	OF 15 2S 36 X 12	36	X	12	-	-	-	-	-	-	-	-	3-2	SUNSET RIDGE DR	BY OTHERS	-	
4-2	R1-1	3	OF 15 2S 30 X 30	30	X	30	6.25	-	-	-	-	-	1	-	-	STOP	-	-	
4-3	D3-1	4	OF 15 2S 36 X 12	36	X	12	-	-	-	-	-	-	-	-	4-2	MENOMONEE AVE	BY OTHERS	-	
4-4	D3-1	4	OF 15 2S 36 X 12	36	X	12	-	-	-	-	-	-	-	-	4-2	SCHNEIDER DR	BY OTHERS	-	
4-8	R2-1	4	OF 15 2S 24 X 30	24	X	30	5.00	-	-	-	1	-	-	-	-	SPEED LIMIT 30 MPH	-	-	
4-9	W11-2	4	OF 15 2S 30 X 30	30	X	30	-	6.25	-	-	1	-	-	-	-	PEDESTRIAN CROSSING SYMBOL	-	-	
4-10	S16-7L	4	OF 15 1 24 X 12	24	X	12	-	2.00	-	-	-	-	-	-	4-9	LEFT DIAGONAL DOWNWARD POINTING ARROW (FLUORESCENT,	-	-	
4-11	W11-2	4	OF 15 2S 30 X 30	30	X	30	-	6.25	-	-	1	-	-	-	-	PEDESTRIAN CROSSING SYMBOL	-	-	
4-12	S16-9P	4	OF 15 1 24 X 12	24	X	12	-	2.00	-	-	-	-	-	-	4-11	AHEAD PLAQUE (FLUORESCENT YELLOW GREEN)	-	-	
4-13	W11-2	4	OF 15 2S 30 X 30	30	X	30	-	6.25	-	-	1	-	-	-	-	PEDESTRIAN CROSSING SYMBOL	-	-	
4-14	S16-7L	4	OF 15 1 24 X 12	24	X	12	-	2.00	-	-	-	-	-	-	4-13	LEFT DIAGONAL DOWNWARD POINTING ARROW (FLUORESCENT,	-	-	
4-15	W11-2	4	OF 15 2S 30 X 30	30	X	30	-	6.25	-	-	1	-	-	-	-	PEDESTRIAN CROSSING SYMBOL	-	-	
4-16	S16-9P	4	OF 15 1 24 X 12	24	X	12	-	2.00	-	-	-	-	-	-	4-15	AHEAD PLAQUE (FLUORESCENT YELLOW GREEN)	-	-	
5-1	R1-1	5	OF 15 2S 30 X 30	30	X	30	6.25	-	-	-	-	-	1	-	-	STOP	-	-	
5-2	D3-1	5	OF 15 2S 36 X 12	36	X	12	-	-	-	-	-	-	-	-	5-1	MENOMONEE AVE	BY OTHERS	-	
5-3	D3-1	5	OF 15 2S 36 X 12	36	X	12	-	-	-	-	-	-	-	-	5-1	WESTBRIDGE AVE	BY OTHERS	-	
5-4	R2-1	5	OF 15 2S 24 X 30	24	X	30	5.00	-	-	-	1	-	-	-	-	SPEED LIMIT 30 MPH	-	-	
5-5	R1-1	5	OF 15 2S 30 X 30	30	X	30	6.25	-	-	-	-	-	1	-	-	STOP	-	-	
5-6	D3-1	5	OF 15 2S 36 X 12	36	X	12	-	-	-	-	-	-	-	-	5-5	MENOMONEE AVE	BY OTHERS	-	
5-7	D3-1	5	OF 15 2S 36 X 12	36	X	12	-	-	-	-	-	-	-	-	5-5	ROBERT AVE	BY OTHERS	-	
5-8	R2-1	5	OF 15 2S 24 X 30	24	X	30	5.00	-	-	-	1	-	-	-	-	SPEED LIMIT 30 MPH	-	-	
6-1	R1-1	6	OF 15 2S 30 X 30	30	X	30	6.25	-	-	-	-	-	1	-	-	STOP	-	-	
SUBTOTAL							113.81	53.00	1	14	9								

PERMANENT SIGNING

SIGN #	SIGN CODE	SHEET #	SIGN SIZE	W IN.	X IN.	H IN.	637.2210		637.2230		634.0808		634.0811		634.0814		SIGN MOUNTED ON SAME POST AS #	DESCRIPTION	NOTES
							SIGNS TYPE II REFLECTIVE	SIGNS TYPE II REFLECTIVE	POSTS TUBULAR STEEL 2X2-INCH EACH	POSTS TUBULAR STEEL 2X2-INCH EACH	POSTS TUBULAR STEEL 2X2-INCH EACH	SIGN							
							H SF	F SF	X 8-FT	X 11-FT	X 14-FT	ON SAME							
6-2	D3-1	6	OF 15 2S 36 X 12				-	-	-	-	-	-	-	-	-	6-1	MENOMONEE AVE	BY OTHERS	
6-3	D3-1	6	OF 15 2S 36 X 12				-	-	-	-	-	-	-	-	-	6-1	SHADY LA	BY OTHERS	
6-4	R1-1	6	OF 15 2S 30 X 30				6.25	-	-	-	-	-	-	1	-	-	STOP	-	
6-5	D3-1	6	OF 15 2S 36 X 12				-	-	-	-	-	-	-	-	-	6-4	MENOMONEE AVE	BY OTHERS	
6-6	D3-1	6	OF 15 2S 36 X 12				-	-	-	-	-	-	-	-	-	6-4	ALLEN AVE	BY OTHERS	
6-7	S4-51	6	OF 15 2S 24 X 48				-	8.00	-	-	-	-	1	-	-	-	SCHOOL SPEED LIMIT	-	
6-8	R9-3A	6	OF 15 2S 24 X 24				4.00	-	-	1	-	-	-	-	-	-	NO PEDESTRIAN CROSSING (SYMBOL SIGN)	-	
6-9	R9-3BL	6	OF 15 2S 18 X 12				1.50	-	-	-	-	-	-	-	-	6-8	USE CROSSWALK (LEFT ARROW)	-	
6-10	R9-3A	6	OF 15 2S 24 X 24				4.00	-	-	1	-	-	-	-	-	-	NO PEDESTRIAN CROSSING (SYMBOL SIGN)	-	
6-11	R9-3BR	6	OF 15 2S 18 X 12				1.50	-	-	-	-	-	-	-	-	6-10	USE CROSSWALK (RIGHT ARROW)	-	
6-12	R9-3A	6	OF 15 2S 24 X 24				4.00	-	-	1	-	-	-	-	-	-	NO PEDESTRIAN CROSSING (SYMBOL SIGN)	-	
6-13	R9-3BL	6	OF 15 2S 18 X 12				1.50	-	-	-	-	-	-	-	-	6-12	USE CROSSWALK (LEFT ARROW)	-	
6-14	R9-3A	6	OF 15 2S 24 X 24				4.00	-	-	1	-	-	-	-	-	-	NO PEDESTRIAN CROSSING (SYMBOL SIGN)	-	
6-15	R9-3BR	6	OF 15 2S 18 X 12				1.50	-	-	-	-	-	-	-	-	6-14	USE CROSSWALK (RIGHT ARROW)	-	
7-1	R2-1	7	OF 15 2S 24 X 30				5.00	-	-	-	-	1	-	-	-	-	SPEED LIMIT 30 MPH	-	
7-2	R1-1	7	OF 15 2S 30 X 30				6.25	-	-	-	-	-	1	-	-	-	STOP	-	
7-3	D3-1	7	OF 15 2S 36 X 12				-	-	-	-	-	-	-	-	-	7-2	MENOMONEE AVE	BY OTHERS	
7-4	D3-1	7	OF 15 2S 36 X 12				-	-	-	-	-	-	-	-	-	7-2	LLOYD AVE	BY OTHERS	
7-5	R9-3A	7	OF 15 2S 24 X 24				4.00	-	-	1	-	-	-	-	-	-	NO PEDESTRIAN CROSSING (SYMBOL SIGN)	-	
7-6	R9-3BL	7	OF 15 2S 18 X 12				1.50	-	-	-	-	-	-	-	-	7-5	USE CROSSWALK (LEFT ARROW)	-	
7-7	R9-3A	7	OF 15 2S 24 X 24				4.00	-	-	1	-	-	-	-	-	-	NO PEDESTRIAN CROSSING (SYMBOL SIGN)	-	
7-8	R9-3BR	7	OF 15 2S 18 X 12				1.50	-	-	-	-	-	-	-	-	7-7	USE CROSSWALK (RIGHT ARROW)	-	
8-1	R1-1	8	OF 15 2S 30 X 30				6.25	-	-	-	-	-	1	-	-	-	STOP	-	
8-2	D3-1	8	OF 15 2S 36 X 12				-	-	-	-	-	-	-	-	-	8-1	MENOMONEE AVE	BY OTHERS	
8-3	D3-1	8	OF 15 2S 36 X 12				-	-	-	-	-	-	-	-	-	8-1	SHERIDAN DR	BY OTHERS	
8-4	R2-1	8	OF 15 2S 24 X 30				5.00	-	-	-	-	1	-	-	-	-	SPEED LIMIT 30 MPH	-	
8-5	R1-1	8	OF 15 2S 30 X 30				6.25	-	-	-	-	-	1	-	-	-	STOP	-	
8-6	D3-1	8	OF 15 2S 36 X 12				-	-	-	-	-	-	-	-	-	8-5	MENOMONEE AVE	BY OTHERS	
8-7	D3-1	8	OF 15 2S 36 X 12				-	-	-	-	-	-	-	-	-	8-5	JACOBSON DR	BY OTHERS	
8-8	R2-1	8	OF 15 2S 24 X 30				5.00	-	-	-	-	1	-	-	-	-	SPEED LIMIT 30 MPH	-	
8-9	R9-3A	8	OF 15 2S 24 X 24				4.00	-	-	1	-	-	-	-	-	-	NO PEDESTRIAN CROSSING (SYMBOL SIGN)	-	
8-10	R9-3BL	8	OF 15 2S 18 X 12				1.50	-	-	-	-	-	-	-	-	8-9	USE CROSSWALK (LEFT ARROW)	-	
8-11	R9-3A	8	OF 15 2S 24 X 24				4.00	-	-	1	-	-	-	-	-	-	NO PEDESTRIAN CROSSING (SYMBOL SIGN)	-	
8-12	R9-3BR	8	OF 15 2S 18 X 12				1.50	-	-	-	-	-	-	-	-	8-11	USE CROSSWALK (RIGHT ARROW)	-	
8-13	R9-3A	8	OF 15 2S 24 X 24				4.00	-	-	1	-	-	-	-	-	-	NO PEDESTRIAN CROSSING (SYMBOL SIGN)	-	
8-14	R9-3BL	8	OF 15 2S 18 X 12				1.50	-	-	-	-	-	-	-	-	8-13	USE CROSSWALK (LEFT ARROW)	-	
8-15	R9-3A	8	OF 15 2S 24 X 24				4.00	-	-	1	-	-	-	-	-	-	NO PEDESTRIAN CROSSING (SYMBOL SIGN)	-	
8-16	R9-3BR	8	OF 15 2S 18 X 12				1.50	-	-	-	-	-	-	-	-	8-15	USE CROSSWALK (RIGHT ARROW)	-	
8-17	W11-2	8	OF 15 2S 30 X 30				-	6.25	-	1	-	-	-	-	-	-	PEDESTRIAN CROSSING SYMBOL	-	
8-18	S16-9P	8	OF 15 1 24 X 12				-	2.00	-	-	-	-	-	-	-	8-17	AHEAD PLAQUE (FLUORESCENT YELLOW GREEN)	-	
9-1	S4-51	9	OF 15 2S 24 X 48				-	8.00	-	-	-	-	1	-	-	-	SCHOOL SPEED LIMIT	-	
9-2	R1-1	9	OF 15 2S 30 X 30				6.25	-	-	-	-	-	1	-	-	-	STOP	-	
9-3	D3-1	9	OF 15 2S 36 X 12				-	-	-	-	-	-	-	-	-	9-2	MENOMONEE AVE	BY OTHERS	
9-4	D3-1	9	OF 15 2S 36 X 12				-	-	-	-	-	-	-	-	-	9-2	THEODORE AVE	BY OTHERS	
9-5	R1-1	9	OF 15 2S 30 X 30				6.25	-	-	-	-	-	1	-	-	-	STOP	-	
SUBTOTAL							107.50	24.25	11	3	8								

PERMANENT SIGNING

637.2210 637.2230 634.0808 634.0811 634.0814

SIGN #	SIGN CODE	SHEET #	SIGN SIZE	W IN.	X IN.	H IN.	SIGN TYPE II REFLECTIVE		POSTS TUBULAR STEEL		SIGN MOUNTED ON SAME POST AS #	DESCRIPTION	NOTES	
							H SF	F SF	2X2-INCH X 8-FT EACH	2X2-INCH X 11-FT EACH				2X2-INCH X 14-FT EACH
9-6	D3-1	9 OF 15	2S 36 X 12				-	-	-	-	-	9-5 MENOMONEE AVE	BY OTHERS	
9-7	D3-1	9 OF 15	2S 36 X 12				-	-	-	-	-	9-5 DARDIS AVE	BY OTHERS	
9-8	R9-3A	9 OF 15	2S 24 X 24				4.00	-	1	-	-	-	NO PEDESTRIAN CROSSING (SYMBOL SIGN)	-
9-9	R9-3BR	9 OF 15	2S 18 X 12				1.50	-	-	-	-	9-8 USE CROSSWALK (RIGHT ARROW)	-	
9-10	R9-3A	9 OF 15	2S 24 X 24				4.00	-	1	-	-	-	NO PEDESTRIAN CROSSING (SYMBOL SIGN)	-
9-11	R9-3BL	9 OF 15	2S 18 X 12				1.50	-	-	-	-	9-10 USE CROSSWALK (LEFT ARROW)	-	
9-12	R9-3A	9 OF 15	2S 24 X 24				4.00	-	1	-	-	-	NO PEDESTRIAN CROSSING (SYMBOL SIGN)	-
9-13	R9-3BR	9 OF 15	2S 18 X 12				1.50	-	-	-	-	9-12 USE CROSSWALK (RIGHT ARROW)	-	
9-14	W11-2	9 OF 15	2S 30 X 30				-	6.25	1	-	-	-	PEDESTRIAN CROSSING SYMBOL	-
9-15	S16-7L	9 OF 15	1 24 X 12				-	2.00	-	-	-	9-14 LEFT DIAGONAL DOWNWARD POINTING ARROW (FLUORESCENT,	-	
9-16	W11-2	9 OF 15	2S 30 X 30				-	6.25	1	-	-	-	PEDESTRIAN CROSSING SYMBOL	-
9-17	S16-9P	9 OF 15	1 24 X 12				-	2.00	-	-	-	9-16 AHEAD PLAQUE (FLUORESCENT YELLOW GREEN)	-	
9-18	W11-2	9 OF 15	2S 30 X 30				-	6.25	1	-	-	-	PEDESTRIAN CROSSING SYMBOL	-
9-19	S16-9P	9 OF 15	1 24 X 12				-	2.00	-	-	-	9-18 AHEAD PLAQUE (FLUORESCENT YELLOW GREEN)	-	
10-1	R2-1	10 OF 15	2S 24 X 30				5.00	-	-	1	-	-	SPEED LIMIT 30 MPH	-
10-2	R1-1	10 OF 15	2S 30 X 30				6.25	-	-	-	1	-	STOP	-
10-3	W4-4P	10 OF 15	2S 24 X 12				-	2.00	-	-	-	10-2 CROSS TRAFFIC DOES NOT STOP	-	
10-4	D3-1	10 OF 15	2S 36 X 12				-	-	-	-	-	10-2 MENOMONEE AVE	BY OTHERS	
10-5	D3-1	10 OF 15	2S 36 X 12				-	-	-	-	-	10-2 LAVERGNE AVE	BY OTHERS	
10-6	W11-2	10 OF 15	2S 30 X 30				-	6.25	-	-	1	-	PEDESTRIAN CROSSING SYMBOL	-
10-7	S16-7L	10 OF 15	1 24 X 12				-	2.00	-	-	-	10-6 LEFT DIAGONAL DOWNWARD POINTING ARROW (FLUORESCENT,	-	
10-10	W11-2	10 OF 15	2S 30 X 30				-	6.25	-	-	1	-	PEDESTRIAN CROSSING SYMBOL	-
10-11	S16-7L	10 OF 15	1 24 X 12				-	2.00	-	-	-	10-10 LEFT DIAGONAL DOWNWARD POINTING ARROW (FLUORESCENT,	-	
10-12	R1-1	10 OF 15	2S 30 X 30				6.25	-	-	1	-	-	STOP	-
10-13	W11-2	10 OF 15	2S 30 X 30				-	6.25	-	-	1	-	PEDESTRIAN CROSSING SYMBOL	-
10-14	S16-9P	10 OF 15	1 24 X 12				-	2.00	-	-	-	10-13 AHEAD PLAQUE (FLUORESCENT YELLOW GREEN)	-	
10-15	W4-4P	10 OF 15	2S 24 X 12				-	2.00	-	-	-	10-12 CROSS TRAFFIC DOES NOT STOP	-	
11-1	R1-1	11 OF 15	2S 30 X 30				6.25	-	-	1	-	-	STOP	-
11-2	D3-1	11 OF 15	2S 36 X 12				-	-	-	-	-	11-1 MENOMONEE AVE	BY OTHERS	
11-3	D3-1	11 OF 15	2S 36 X 12				-	-	-	-	-	11-1 HIAWATHA AVE	BY OTHERS	
11-4	R1-1	11 OF 15	2S 30 X 30				6.25	-	-	1	-	-	STOP	-
11-5	D3-1	11 OF 15	2S 36 X 12				-	-	-	-	-	11-4 MENOMONEE AVE	BY OTHERS	
11-6	D3-1	11 OF 15	2S 36 X 12				-	-	-	-	-	11-4 ARTHUR AVE	BY OTHERS	
11-7	R2-1	11 OF 15	2S 24 X 30				5.00	-	-	1	-	-	SPEED LIMIT 30 MPH	-
11-8	R9-3A	11 OF 15	2S 24 X 24				4.00	-	1	-	-	-	NO PEDESTRIAN CROSSING (SYMBOL SIGN)	-
11-9	R9-3BL	11 OF 15	2S 18 X 12				1.50	-	-	-	-	11-8 USE CROSSWALK (LEFT ARROW)	-	
11-10	R9-3A	11 OF 15	2S 24 X 24				4.00	-	1	-	-	-	NO PEDESTRIAN CROSSING (SYMBOL SIGN)	-
11-11	R9-3BR	11 OF 15	2S 18 X 12				1.50	-	-	-	-	11-10 USE CROSSWALK (RIGHT ARROW)	-	
11-12	W11-2	10 OF 15	2S 30 X 30				-	6.25	-	-	1	-	PEDESTRIAN CROSSING SYMBOL	-
11-13	S16-9P	10 OF 15	1 24 X 12				-	2.00	-	-	-	11-12 AHEAD PLAQUE (FLUORESCENT YELLOW GREEN)	-	
12-1	R1-1	12 OF 15	2S 30 X 30				6.25	-	-	1	-	-	STOP	-
12-2	D3-1	12 OF 15	2S 36 X 12				-	-	-	-	-	12-1 MENOMONEE AVE	BY OTHERS	
12-3	D3-1	12 OF 15	2S 36 X 12				-	-	-	-	-	12-1 FLEET AVE	BY OTHERS	
12-4	D3-1	12 OF 15	2S 36 X 12				-	-	-	-	-	12-6 MENOMONEE AVE	BY OTHERS	
12-5	D3-1	12 OF 15	2S 36 X 12				-	-	-	-	-	12-6 TAMARACK DR	BY OTHERS	
SUBTOTAL							68.75	61.75	8	6	5			

PERMANENT SIGNING

SIGN #	SIGN CODE	SHEET #	SIGN SIZE	W IN.	X IN.	H IN.	637.2210	637.2230	634.0808	634.0811	634.0814	SIGN MOUNTED ON SAME POST AS #	DESCRIPTION	NOTES
							SIGNS TYPE II REFLECTIVE H SF	SIGNS TYPE II REFLECTIVE F SF	POSTS TUBULAR STEEL 2X2-INCH X 8-FT EACH	POSTS TUBULAR STEEL 2X2-INCH X 11-FT EACH	POSTS TUBULAR STEEL 2X2-INCH X 14-FT EACH			
12-6	R1-1	12 OF 15	2S 30 X 30				6.25	-	-	1	-	-	STOP	-
12-7	R9-3A	12 OF 15	2S 24 X 24				4.00	-	1	-	-	-	NO PEDESTRIAN CROSSING (SYMBOL SIGN)	-
12-8	R9-3BL	12 OF 15	2S 18 X 12				1.50	-	-	-	-	12-7	USE CROSSWALK (LEFT ARROW)	-
12-9	R9-3A	12 OF 15	2S 24 X 24				4.00	-	1	-	-	-	NO PEDESTRIAN CROSSING (SYMBOL SIGN)	-
12-10	R9-3BR	12 OF 15	2S 18 X 12				1.50	-	-	-	-	12-9	USE CROSSWALK (RIGHT ARROW)	-
13-1	R2-1	13 OF 15	2S 24 X 30				5.00	-	-	1	-	-	SPEED LIMIT 30 MPH	-
15-1	R2-1	15 OF 15	2S 24 X 30				5.00	-	-	1	-	-	SPEED LIMIT 30 MPH	-
15-2	R10-7	15 OF 15	2S 24 X 30				5.00	-	-	-	-	-	DO NOT BLOCK INTERSECTION	-
15-3	R3-17	15 OF 15	2S 30 X 24				5.00	-	-	-	-	-	BIKE LANE	-
15-4	R3-17B	15 OF 15	2S 30 X 12				2.50	-	-	-	-	15-3	ENDS	-
SUBTOTAL							39.75	0.00	2	3	0			
TOTAL 0010							329.81	139.00	22	26	22			

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SIGN REMOVALS

SIGN #	SIGN CODE	SHEET #		638.2602	638.3000	DESCRIPTION	NOTES
				REMOVING SIGNS TYPE II EACH	REMOVING SMALL SIGN SUPPORTS EACH		
R1-1	R1-1	1	OF 15	1	1	STOP	-
R1-2	R1-1	1	OF 15	1	-	STOP	-
R1-3	R1-3P	1	OF 15	1	-	4 WAY	-
R1-4	D9-2	1	OF 15	1	1	HOSPITAL	INCLUDES TEXT AND ARROW
R1-6	S1-1	1	OF 15	1	1	SCHOOL ADVANCE	-
R1-7	S16-9P	1	OF 15	1	-	AHEAD PLAQUE (FLUORESCENT YELLOW GREEN)	-
R1-8	R7-1D	1	OF 15	1	1	NO PARKING ANY TIME - DOUBLE ARROW	-
R1-9	R7-1D	1	OF 15	1	1	NO PARKING ANY TIME - DOUBLE ARROW	-
R1-10	R5-2	1	OF 15	1	1	NO TRUCKS SYMBOL	-
R1-11	R12-1	1	OF 15	1	-	WEIGHT LIMIT 3 TONS	-
R1-12	D3-1	1	OF 15	1	-	MENOMONEE AVE	-
R1-13	D3-1	1	OF 15	1	-	SHADY LA	-
R1-14	R1-1	1	OF 15	1	1	STOP	-
R2-1	R3-8LTR	2	OF 15	1	1	LEFT ONLY, THROUGH-RIGHT	-
R2-2	W3-1	2	OF 15	1	1	STOP AHEAD	-
R2-3	R1-1	2	OF 15	1	1	STOP	-
R2-4	D3-1	2	OF 15	1	-	MENOMONEE AVE	-
R2-5	D3-1	2	OF 15	1	-	LYNWOOD DR	-
R2-6	R2-1	2	OF 15	1	-	SPEED LIMIT 30 MPH	-
R3-1	R3-1	3	OF 15	1	1	NO RIGHT TURN SYMBOL	-
R3-2	R1-1	3	OF 15	1	1	STOP	-
R3-3	D3-1	3	OF 15	1	-	MENOMONEE AVE	-
R3-4	D3-1	3	OF 15	1	-	SUNSET RIDGE DR	-
R3-5	R7-2	3	OF 15	1	1	NO PARKING (TIME) TO (TIME)	-
R4-1	R7-2	4	OF 15	1	1	NO PARKING (TIME) TO (TIME)	-
R4-2	R1-1	4	OF 15	1	1	STOP	-
R4-3	D3-1	4	OF 15	1	-	MENOMONEE AVE	-
R4-4	D3-1	4	OF 15	1	-	SCHNEIDER DR	-
R4-5	R7-1D	4	OF 15	1	1	NO PARKING ANY TIME - DOUBLE ARROW	-
R4-6	R7-1D	4	OF 15	1	1	NO PARKING ANY TIME - DOUBLE ARROW	-
R4-7	R7-2	4	OF 15	1	1	NO PARKING (TIME) TO (TIME)	-
R4-8	R2-1	4	OF 15	1	-	SPEED LIMIT 30 MPH	-
R5-1	R1-1	5	OF 15	1	1	STOP	-
R5-2	D3-1	5	OF 15	1	-	MENOMONEE AVE	-
R5-3	D3-1	5	OF 15	1	-	WESTBRIDGE AVE	-
R5-4	R7-2	5	OF 15	1	1	NO PARKING (TIME) TO (TIME)	-
R5-5	R2-1	5	OF 15	1	1	SPEED LIMIT 30 MPH	-
R5-6	R7-2	5	OF 15	1	1	NO PARKING (TIME) TO (TIME)	-
R5-7	R1-1	5	OF 15	1	1	STOP	-
R5-8	D3-1	5	OF 15	1	-	MENOMONEE AVE	-
R5-9	D3-1	5	OF 15	1	-	ROBERT AVE	-
R5-10	R7-2	5	OF 15	1	1	NO PARKING (TIME) TO (TIME)	-
R5-11	R7-2	5	OF 15	1	1	NO PARKING (TIME) TO (TIME)	-
R5-12	R2-1	5	OF 15	1	-	SPEED LIMIT 30 MPH	-
R6-1	R1-1	6	OF 15	1	1	STOP	-
SUBTOTAL				45	26		

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SIGN REMOVALS

SIGN #	SIGN CODE	SHEET #		638.2602	638.3000	DESCRIPTION	NOTES
				REMOVING SIGNS TYPE II EACH	REMOVING SMALL SIGN SUPPORTS EACH		
R6-2	D3-1	6	OF 15	1	-	MENOMONEE AVE	-
R6-3	D3-1	6	OF 15	1	-	SHADY LA	-
R6-4	R1-1	6	OF 15	1	1	STOP	-
R6-5	D3-1	6	OF 15	1	-	MENOMONEE AVE	-
R6-6	D3-1	6	OF 15	1	-	ALLEN AVE	-
R6-7	S4-51	6	OF 15	1	-	SCHOOL SPEED LIMIT	-
R7-1	R2-1	7	OF 15	1	1	SPEED LIMIT 30 MPH	-
R7-2	R1-1	7	OF 15	1	1	STOP	-
R7-3	D3-1	7	OF 15	1	-	MENOMONEE AVE	-
R7-4	D3-1	7	OF 15	1	-	LLOYD AVE	-
R7-5	R7-1D	7	OF 15	1	1	NO PARKING ANYTIME - DOUBLE ARROW	-
R8-1	R1-1	8	OF 15	1	1	STOP	-
R8-2	D3-1	8	OF 15	1	-	MENOMONEE AVE	-
R8-3	D3-1	8	OF 15	1	-	SHERIDAN DR	-
R8-4	R2-1	8	OF 15	1	1	SPEED LIMIT 30 MPH	-
R8-5	R1-1	8	OF 15	1	1	STOP	-
R8-6	D3-1	8	OF 15	1	-	MENOMONEE AVE	-
R8-7	D3-1	8	OF 15	1	-	JACOBSON DR	-
R8-8	R2-1	8	OF 15	1	1	SPEED LIMIT 30 MPH	-
R9-1	S4-51	9	OF 15	1	1	SCHOOL SPEED LIMIT	-
R9-2	R1-1	9	OF 15	1	1	STOP	-
R9-3	D3-1	9	OF 15	1	1	MENOMONEE AVE	-
R9-4	D3-1	9	OF 15	1	1	THEODORE AVE	-
R9-5	R1-1	9	OF 15	1	1	STOP	-
R9-6	D3-1	9	OF 15	1	-	MENOMONEE AVE	-
R9-7	D3-1	9	OF 15	1	-	DARDIS AVE	-
R9-8	R7-1L	9	OF 15	1	1	NO PARKING ANYTIME - LEFT ARROW	-
R10-1	R2-1	10	OF 15	1	-	SPEED LIMIT 30 MPH	-
R10-2	R1-1	10	OF 15	1	1	STOP	-
R10-3	W4-4P	10	OF 15	1	-	CROSS TRAFFIC DOES NOT STOP	-
R10-4	D3-1	10	OF 15	1	-	MENOMONEE AVE	-
R10-5	D3-1	10	OF 15	1	-	LAVERGNE AVE	-
R10-6	W11-2	10	OF 15	1	1	PEDESTRIAN CROSSING SYMBOL	-
R10-7	S16-7L	10	OF 15	1	-	LEFT DIAGONAL DOWNWARD POINTING ARROW (FLUORESCENT)	-
R10-8	W11-2	10	OF 15	1	1	PEDESTRIAN CROSSING SYMBOL	-
R10-9	S16-9P	10	OF 15	1	-	AHEAD PLAQUE (FLUORESCENT YELLOW GREEN)	-
R10-10	W11-2	10	OF 15	1	1	PEDESTRIAN CROSSING SYMBOL	-
R10-11	S16-7L	10	OF 15	1	-	LEFT DIAGONAL DOWNWARD POINTING ARROW (FLUORESCENT)	-
R10-12	R1-1	10	OF 15	1	1	STOP	-
R11-1	R1-1	11	OF 15	1	1	STOP	-
R11-2	D3-1	11	OF 15	1	-	MENOMONEE AVE	-
R11-3	D3-1	11	OF 15	1	-	HIAWATHA AVE	-
R11-4	R1-1	11	OF 15	1	1	STOP	-
R11-5	D3-1	11	OF 15	1	-	MENOMONEE AVE	-
R11-6	D3-1	11	OF 15	1	-	ARTHUR AVE	-
SUBTOTAL				43	21		

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SIGN REMOVALS

SIGN #	SIGN CODE	SHEET #		638.2602	638.3000	DESCRIPTION	NOTES
				REMOVING SIGNS TYPE II EACH	REMOVING SMALL SIGN SUPPORTS EACH		
R11-7	R9-3BR	11 OF 15		1	-	USE CROSSWALK (RIGHT ARROW)	-
R12-1	R1-1	12 OF 15		1	-	STOP	-
R12-2	D3-1	12 OF 15		1	-	MENOMONEE AVE	-
R12-3	D3-1	12 OF 15		1	-	FLEET AVE	-
R12-4	D3-1	12 OF 15		1	-	MENOMONEE AVE	-
R12-5	D3-1	12 OF 15		1	-	TAMARACK DR	-
R12-6	R1-1	12 OF 15		1	1	STOP	-
R13-1	R2-1	13 OF 15		1	1	SPEED LIMIT 30 MPH	-
R14-1	-	15 OF 15		1	1	CUSTOM STREET AND STOP SIGNS	REMOVE, SALVAGE, REINSTALL SIGNS + POST
R15-1	R7-1R	15 OF 15		1	1	NO PARKING ANY TIME - RIGHT ARROW	-
R15-2	R2-1	15 OF 15		1	1	SPEED LIMIT 30 MPH	-
R15-3	R7-1D	15 OF 15		1	-	NO PARKING ANY TIME - DOUBLE ARROW	-
R15-4	R7-1L	15 OF 15		1	1	NO PARKING ANY TIME - LEFT ARROW	-
R15-5	-	15 OF 15		1	1	CUSTOM STREET SIGNS	REMOVE, SALVAGE, REINSTALL SIGNS + POST
R15-6	R10-7	15 OF 15		1	1	DO NOT BLOCK INTERSECTION	-
R15-7	R7-1D	15 OF 15		1	-	NO PARKING ANY TIME - DOUBLE ARROW	-
SUBTOTAL				11	7		
TOTAL 0010				99	54		

STRUCTURE B-67-401 SUMMARY OF QUANTITIES (FOR INFORMATION ONLY)

BID ITEM NUMBER	BID ITEMS	CATEGORY 0020	UNIT	TOTAL
206.2001	EXCAVATION FOR STRUCTURES CULVERTS (B-67-401)	0020	EACH	1
210.2500	BACKFILL STRUCTURE TYPE B	0020	TON	1,432
311.0110	BREAKER RUN	0020	TON	234
504.0100	CONCRETE MASONRY CULVERTS	0020	CY	196
505.0400	BAR STEEL REINFORCEMENT HS STRUCTURES	0020	LB	26,890
505.0600	BAR STEEL REINFORCEMENT HS COATED STRUCTURES	0020	LB	860
511.1200	TEMPORARY SHORING (B-67-401)	0020	SF	800
513.2001	RAILING PIPE	0020	LF	44
516.0500	RUBBERIZED MEMBRANE WATERPROOFING	0020	SY	45
606.0300	RIPRAP HEAVY	0020	CY	81
645.0105	GEOTEXTILE TYPE C	0020	SY	292
645.0120	GEOTEXTILE TYPE HR	0020	SY	121
SPV.0195.01	BOX CULVERT FILL MATERIAL	0020	TON	80
	NON-BID ITEM			
	FILLER		SIZE	3/4"

R/W PROJECT NUMBER 2720-07-01	SHEET NUMBER 4.01	TOTAL SHEETS 14
FEDERAL PROJECT NUMBER		
PLAT OF RIGHT OF WAY REQUIRED FOR VILLAGE OF MEMOMONEE FALLS, MEMOMONEE AVENUE TOWN HALL ROAD TO STH 175		
MEMOMONEE AVENUE		WAUKESHA COUNTY
CONSTRUCTION PROJECT NUMBER 2720-07-71		

CONVENTIONAL SYMBOLS

SECTION LINE		SECTION CORNER SYMBOL		R/W MONUMENT (TO BE SET)	
QUARTER LINE		SECTION CORNER MONUMENT		NON-MONUMENTED R/W POINT	
SIXTEENTH LINE		SECTION CORNER MONUMENT		FOUND IRON PIN (1-INCH UNLESS NOTED)	
NEW REFERENCE LINE		GEODETIC SURVEY MONUMENT		SIXTEENTH CORNER MONUMENT	
NEW R/W LINE		SIGN		OFF-PREMISE SIGN	
EXISTING R/W OR HE LINE		PARALLEL OFFSETS			
PROPERTY LINE					
LOT, TIE, & OTHER MINOR LINES					
SLOPE INTERCEPT					
CORPORATE LIMITS					
UNDERGROUND FACILITY (COMMUNICATIONS, ELECTRIC, ETC)					
NEW R/W (FEE OR HE) (HATCHING VARIES BY OWNER)					
TEMPORARY LIMITED EASEMENT AREA					
EASEMENT AREA (PERMANENT LIMITED OR RESTRICTED DEVELOPMENT)					
TRANSMISSION STRUCTURES					
BUILDING					
BRIDGE					
CULVERT					

CONVENTIONAL ABBREVIATIONS

ACCESS RIGHTS	AR	OUTLOT	OL
ACRES	AC	PAGE	P
AHEAD	AH	POINT OF TANGENCY	PT
ALUMINUM	ALUM	PROPERTY LINE	PL
AND OTHERS	ET AL	RECORDED AS (100')	
BACK	BK	REEL / IMAGE	R/I
BLOCK	BLK	REFERENCE LINE	R/L
CENTERLINE	C/L	PERMANENT LIMITED EASEMENT	PLE
CERTIFIED SURVEY MAP	CSM	POINT OF BEGINNING	POB
CONCRETE	CONC	POINT OF CURVATURE	PC
COUNTY	CO	POINT OF COMPOUND CURVE	PCC
COUNTY TRUNK HIGHWAY	CTH	POINT OF INTERSECTION	PI
DISTANCE	DIST	REMAINING	REM
CORNER	COR	RESTRICTIVE DEVELOPMENT EASEMENT	RDE
DOCUMENT NUMBER	DOC	RIGHT	RT
EASEMENT	EASE	RIGHT OF WAY	R/W
EXISTING	EX	SECTION	SEC
GAS VALVE	GV	SEPTIC VENT	SEPV
GRID NORTH	GN	SQUARE FEET	SF
HIGHWAY EASEMENT	HE	STATE TRUNK HIGHWAY	STH
IDENTIFICATION	ID	STATION	STA
LAND CONTRACT	LC	TELEPHONE PEDESTAL	TP
LEFT	LT	TEMPORARY LIMITED EASEMENT	TLE
MONUMENT	MON	TRANSPORTATION PROJECT PLAT	TPP
NATIONAL GEODETIC SURVEY NUMBER	NGS	UNITED STATES HIGHWAY	USH
	NO	VOLUME	V

CONVENTIONAL UTILITY SYMBOLS

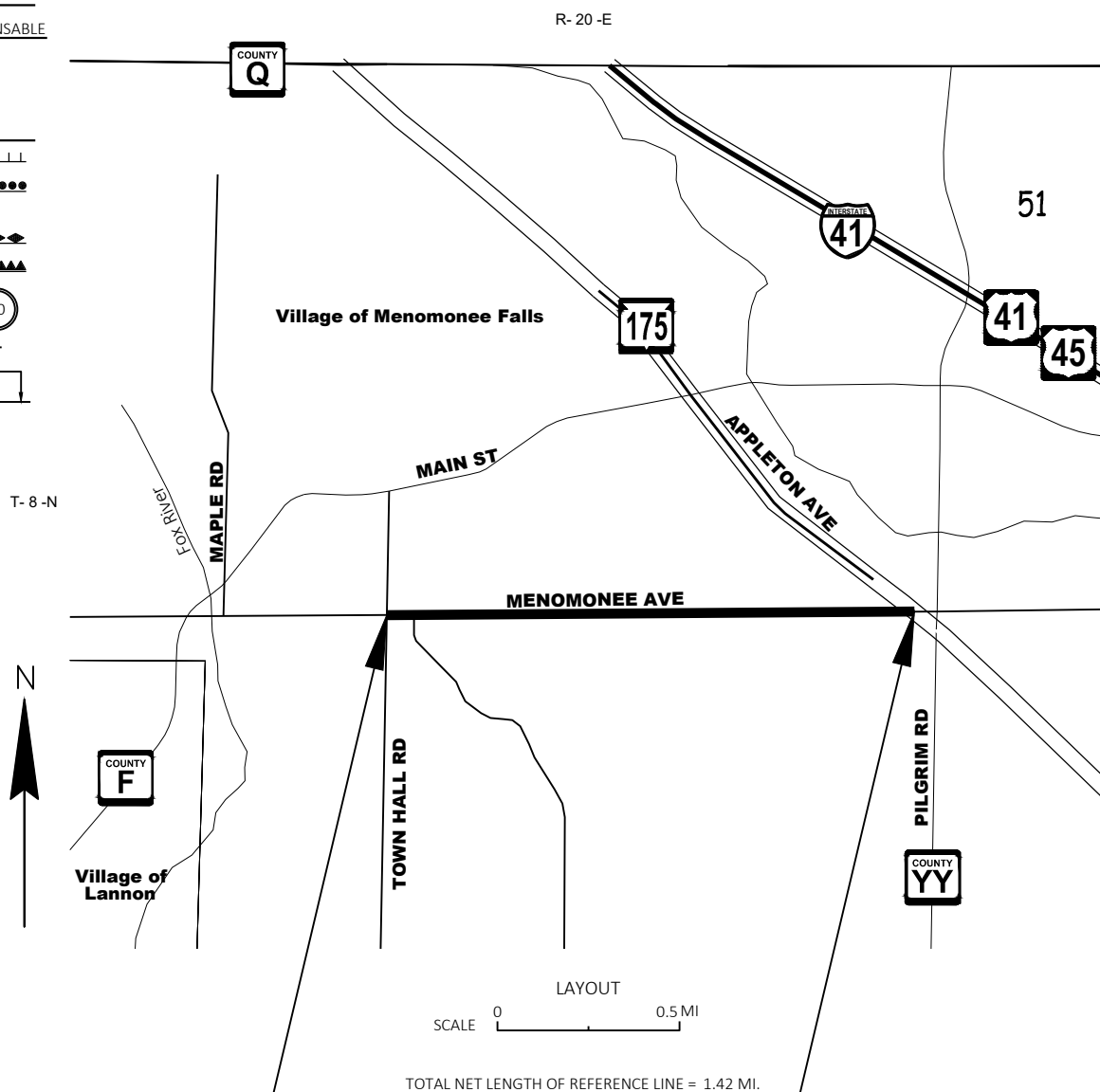
WATER		NON-COMPENSABLE		COMPENSABLE	
GAS					
TELEPHONE					
OVERHEAD TRANSMISSION LINES					
ELECTRIC					
CABLE TELEVISION					
FIBER OPTIC					
SANITARY SEWER					
STORM SEWER					
ELECTRIC TOWER					
POWER POLE					
TELEPHONE POLE					
TELEPHONE PEDESTAL					

CURVE DATA ABBREVIATIONS

LONG CHORD	LCH
LONG CHORD BEARING	LCB
RADIUS	R
DEGREE OF CURVE	D
CENTRAL ANGLE	Δ / DELTA
LENGTH OF CURVE	L
TANGENT	T
DIRECTION AHEAD	DA
DIRECTION BACK	DB

BEGIN RELOCATION ORDER
STA 100+00.00
 Y=432,058.25
 X=2,466,350.14
 6.00 FT SOUTH AND 0.02 FT WEST OF THE CENTER CORNER OF SEC. 9, T8N, R20E

END RELOCATION ORDER
STA 175+18.46
 Y=432,258.35
 X=2,473,865.16
 6.98 FT SOUTH AND 424.43 FT WEST OF THE EAST CORNER OF SEC. 10, T8N, R20E



NOTES:

- POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN STATE PLANE COORDINATE SYSTEM (WSPCS), SOUTH ZONE, NAD83 (2011) IN US SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.
- ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 3/4" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.
- ALL RIGHT-OF-WAY LINES DEPICTED IN THE NON-ACQUISITION AREAS ARE INTENDED TO RE-ESTABLISH EXISTING RIGHT-OF-WAY LINES AS DETERMINED FROM PREVIOUS PROJECTS, OTHER RECORDED DOCUMENTS, CENTERLINE OF EXISTING PAVEMENTS AND/OR EXISTING OCCUPATIONAL LINES.
- RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY OR OTHER SURVEYS OF PUBLIC RECORD.
- FOR CURRENT ACCESS/DRIVEWAY INFORMATION, CONTACT THE VILLAGE OF MEMOMONEE FALLS.
- DIMENSIONING FOR THE NEW RIGHT-OF-WAY IS MEASURED ALONG AND PERPENDICULAR TO NEW REFERENCE LINES.
- A TEMPORARY LIMITED EASEMENT (TLE) IS A RIGHT FOR CONSTRUCTION PURPOSES, AS DEFINED HEREIN, INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON, THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM DESIRABLE. ALL (TLEs) ON THIS PLAT EXPIRE AT THE COMPLETION OF THE CONSTRUCTION PROJECT FOR WHICH THIS INSTRUMENT IS GIVEN.
- A PERMANENT LIMITED EASEMENT (PLE) IS A RIGHT FOR CONSTRUCTION AND MAINTENANCE PURPOSES, AS DEFINED HEREIN, INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON AND THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM DESIRABLE, BUT WITHOUT PREJUDICE TO THE OWNER'S RIGHTS TO MAKE OR CONSTRUCT IMPROVEMENTS ON SAID LANDS OR TO FLATTEN THE SLOPES, PROVIDING SAID ACTIVITIES WILL NOT IMPAIR OR OTHERWISE ADVERSELY AFFECT THE HIGHWAY FACILITIES.
- PROPERTY LINES SHOWN ON THIS PLAT FOR PROPERTIES BEING IMPACTED ARE DRAWN FROM DATA DERIVED FROM FILED/RECORDED MAPS AND DOCUMENTS OF PUBLIC RECORD. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.
- INFORMATION FOR THE BASIS OF EXISTING HIGHWAY RIGHT-OF-WAY POINTS OF REFERENCE AND ACCESS CONTROL ARE LISTED ON DETAIL SHEETS.

ORIGINAL PLAT PREPARED BY

DAVID A. YURK
S-2648
OSHKOSH
WI

DATE: 10/6/2023 *David A. Yurk*
LAND SURVEYOR

THIS PLAT IS A GRAPHIC REPRESENTATION AND IS FOR REFERENCE PURPOSES ONLY. DEEDS MUST BE CHECKED TO DETERMINE PROPERTY BOUNDARIES AND ACCESS RIGHTS.

REVISION DATE
7/16/2024 (2)
9/9/2024 (3)

APPROVED FOR THE VILLAGE OF MEMOMONEE FALLS
 DATE: 11/6/23 *Thomas W. Hoffman*
 (SIGNATURE)
 THOMAS W. HOFFMAN
 (PRINTED NAME)

SCHEDULE OF LANDS & INTERESTS REQUIRED

OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY, AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTEREST TO THE VILLAGE OF MENOMONEE FALLS.

PARCEL NUMBER	SHEET NUMBER	OWNER(S)	INTEREST(S) REQUIRED	R/W SQUARE FEET REQUIRED			PLE (S.F.)	TLE (S.F.)
				NEW	EXISTING	TOTAL		
1	4.05	VILLAGE OF MENOMONEE FALLS KIWANIS PARK	TLE	-	-	-	-	7,464
2	4.05	SUSAN A. BATES	TLE	-	-	-	-	1,852
3	4.05	FELIX D. & DONNA L. SKARDA	TLE	-	-	-	-	1,613
4	4.05	ALEXANDER S. & JESSICA S. RANDAZZO	TLE	-	-	-	-	502
6	4.05	SCHMIDT & BARTELT, INC.	TLE	-	-	-	-	1,580
7	4.05	VILLAGE OF MENOMONEE FALLS	TLE	-	-	-	-	5,542
8	4.05	MALLARD COVE LIMITED PARTNERSHIP	PLE, TLE	-	-	-	1,657	12,095
9	4.06	MICHAEL L. & JODY K. GERSONDE	TLE	-	-	-	-	977
11	4.06	JOHN & SHELLY ESSER	TLE	-	-	-	-	1,051
12	4.06	MICHAEL S. & ALINA C. BOENITZ	TLE	-	-	-	-	1,051
13	4.06	MICHAEL D. & GAIL ROHDE	TLE	-	-	-	-	1,612
14	4.06	JENNIFER M. & ERROL J. MEINHOLZ	TLE	-	-	-	-	1,001
16	4.06	TROY R. & EMILY O. SCHMITT	TLE	-	-	-	-	471
17	4.06	RICHARD D. WEBER	TLE	-	-	-	-	720
18	4.06	HUNTER L. ALDERTON & MORGAN R. STERNAD	TLE	-	-	-	-	999
19	4.06	VILLAGE OF MENOMONEE FALLS	TLE	-	-	-	-	752
21	4.06, 4.07 & 4.08	YMCA OF CENTRAL WAUKESHA COUNTY, INC.	TLE	-	-	-	-	8,519
22	4.07	KAREN L. KORALEWSKI	TLE	-	-	-	-	1,291
23	4.07	JENNIFER G. LAMB & TIMOTHY P. ASPENSON	TLE	-	-	-	-	1,291
24	4.07	TODD F. & LAURA A. CLARK	TLE	-	-	-	-	960
26	4.07	KATIE M. MUYLEART	TLE	-	-	-	-	1,385
27	4.07	MARK FRANZOWIAK & JULIANNE KOEHN	TLE	-	-	-	-	1,117
28	4.08	SAMANTHA M. THOMPSON	TLE	-	-	-	-	1,069
29	4.08	ROBERT W. & JANICE E. BRAUN	TLE	-	-	-	-	389
31	4.08	MICHAEL L. FUGATE	TLE	-	-	-	-	625
32	4.08	ABIGAIL LYNN TANNER	TLE	-	-	-	-	90
33	4.08	N84W17147 MENOMONEE, LLC	TLE	-	-	-	-	4,000
34	4.08 & 4.09	MENOMONEE PROPERTY HOLDINGS, LLC.	TLE	-	-	-	-	1,550
35	4.08	ESSER HOMESTEAD TRUST OF MARCH 7, 2016	TLE	-	-	-	-	330
36	4.09	ZACHARY B. & STEPHANIE L. ILER	TLE	-	-	-	-	35
37	4.09	PATRICK H. & PEGGY J. REMFREY, TRUSTEES OF THE REMFREY TRUST DATED FEBRUARY 22, 2016	TLE	-	-	-	-	616
38	4.09	JOSEPH & DIANE MARISCAL AND TATE MARISCAL, ALL AS JOINT TENANTS	TLE	-	-	-	-	575
39	4.09	VILLAGE OF MENOMONEE FALLS	TLE	-	-	-	-	400
40	4.09	BRADLY T. KREGER & CHRISTINA M. MANEY	TLE	-	-	-	-	150
41	4.09	SOLOMON REAL ESTATE HOLDINGS, LLC.	TLE	-	-	-	-	1,200
42	4.09 & 4.10	G&E HEALTHCARE REIT WISCONSIN MOB PORTFOLIO, LLC.	TLE	-	-	-	-	3,820
43	4.10	CHARLES & LEORA S. KRISIK, TRUSTEES OF THE KRISIK TRUST DATED APRIL 26, 2010	TLE	-	-	-	-	1,050
44	4.10	SCOTT E. REHN	FEE, TLE	6	-	6	-	1,543
46	4.10	DUSTIN R. REINHOLTZ	TLE	-	-	-	-	1,498
47	4.10	KEVIN & THERESA DUMAS	TLE	-	-	-	-	1,003
48	4.10	DARRIN CHRISTENSEN	TLE	-	-	-	-	1,040
49	4.10	EMMANUEL COMMUNITY EVANGELICAL UNITED BROTHERS CONGREGATION OF MENOMONEE FALLS, WISCONSIN	TLE	-	-	-	-	3,174
51	4.10	JOINT SCHOOL DISTRICT NO. 1	FEE, TLE	61	13,013	13,074	-	2,344
52	4.11	HOLLY A. DRACE	TLE	-	-	-	-	780
53	4.11	COLLEEN LABLANC	TLE	-	-	-	-	308
54	4.11	KODI GISSIBL	TLE	-	-	-	-	650
56	4.11	JANET M. CHEESEMAN	TLE	-	-	-	-	1,000
57	4.11	THE RECTOR, WARDENS AND VESTRYMEN OF ST. FRANCIS' EPISCOPAL CHURCH	FEE, TLE	74	10,759	10,833	-	1,351
58	4.11	ERIC J. & JENNIFER M. GOSS	TLE	-	-	-	-	855
59	4.11	JOSEPH JD PHILLIPS & MEGAN R. FISH	TLE	-	-	-	-	1,046
61	4.11	LAUREL E. ADRAKTAS AND HER SUCCESSORS AS TRUSTEE OF THE LAUREL E. ADRAKTAS REVOCABLE TRUST DATED SEPTEMBER 2, 2016	TLE	-	-	-	-	1,391
62	4.11 & 4.12	JANET LEE GRIESBACH	TLE	-	-	-	-	1,145
63	4.12	JOEL F. & DIANE M. BECKER	TLE	-	-	-	-	512
64	4.12	KEVIN P. & MELISSA L. OLYNICK	TLE	-	-	-	-	512
65	4.12	BRYAN R. TROYER	TLE	-	-	-	-	320
66	4.12	JACQUELINE G. YUARTE	TLE	-	-	-	-	280
67	4.12	CHRISTOPHER GULLICKSEN	TLE	-	-	-	-	630
68	4.12	SUSAN DELUCA	TLE	-	-	-	-	280
69	4.12	LUKE NIENOW	TLE	-	-	-	-	336

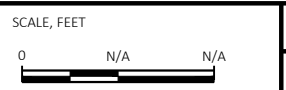
UTILITY NUMBER	SHEET NUMBER	OWNER	INTEREST REQUIRED
100	4.05 & 4.11	WE ENERGIES - GAS	RELEASE OF RIGHTS
101	4.05	SCHOOL DISTRICT OF MENOMONEE FALLS	RELEASE OF RIGHTS
102	4.05	EVERSTREAM	RELEASE OF RIGHTS
103	4.05, 4.07, 4.08, 4.09, 4.10, 4.11, 4.13 & 4.14	AT&T WISCONSIN	RELEASE OF RIGHTS
104	4.05, 4.06, 4.07, 4.08, 4.09, 4.10, 4.11, 4.12, 4.13 & 4.14	WE ENERGIES - ELECTRIC	RELEASE OF RIGHTS
105	4.07 & 4.14	CHARTER COMMUNICATIONS	RELEASE OF RIGHTS
106	4.14	MCI-VERIZON	RELEASE OF RIGHTS
107	4.05, 4.06 & 4.08	MENOMONEE FALLS TELEPHONE COMPANY	RELEASE OF RIGHTS
108	4.08	SPECTRUM MID-AMERICA, LLC.	RELEASE OF RIGHTS
109	4.06, 4.08, 4.09, 4.10, 4.11, 4.12 & 4.14	VILLAGE OF MENOMONEE FALLS - SANITARY	RELEASE OF RIGHTS
110	4.05, 4.06, 4.07, 4.08, 4.09, 4.10 & 4.11	VILLAGE OF MENOMONEE FALLS	RELEASE OF RIGHTS
111	4.12 & 4.14	VILLAGE OF MENOMONEE FALLS - WATERMAIN	RELEASE OF RIGHTS

4

4

REVISION DATE	DATE	SCALE, FEET	HWY:	STATE R/W PROJECT NUMBER:	PLAT SHEET
7/16/2024 (2)	10/06/2023	0 N/A N/A	MENOMONEE AVE	2720-07-01	4.02
9/9/2024 (3)	GRID FACTOR		WISCONSIN	CONSTRUCTION PROJECT NUMBER:	PS&E SHEET
	N/A			2720-07-71	

DATE	10/06/2023
GRID FACTOR	N/A



HWY:	MENOMONEE AVE
COUNTY:	WISCONSIN

STATE R/W PROJECT NUMBER:	2720-07-01
CONSTRUCTION PROJECT NUMBER:	2720-07-71

PLAT SHEET	4.02
PS&E SHEET	

E

SCHEDULE OF LANDS & INTERESTS REQUIRED

OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY, AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTEREST TO THE VILLAGE OF MENOMONEE FALLS.

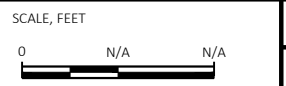
PARCEL NUMBER	SHEET NUMBER	OWNER(S)	INTEREST(S) REQUIRED	R/W SQUARE FEET REQUIRED			PLE (S,F,)	TLE (S,F,)
				NEW	EXISTING	TOTAL		
70	4.12	RICHMOND L. SCHNEIDER & KATHLEEN M. SCHNEIDER	TLE	-	-	-	-	826
71	4.12	MAI Z. VANG & CHAO THAO	TLE	-	-	-	-	905
72	4.12	THERESA KEENLANZE	TLE	-	-	-	-	720
73	4.12	CHERYL E. PIZZOFERRATO	TLE	-	-	-	-	720
74	4.12	ANTHONY J. & NICOLE M. KOENINGS	TLE	-	-	-	-	950
75	4.12	WILLIAM F. HOWARD & TRACY A. HOWARD	TLE	-	-	-	-	826
76	4.12	JAMES F. & TERESA J. JANNKE	TLE	-	-	-	-	1,300
77 (2)	4.12	ELIMINATED	-	-	-	-	-	-
78	4.12	SCOTT W. & DONNA J. OLSON	TLE	-	-	-	-	827
79	4.13	ALDO GARCIA COLUNGA	TLE	-	-	-	-	585
80	4.12 & 4.13	CORY RABE & DANIELLE RABE	TLE	-	-	-	-	826
81	4.13	JESSICA L. STROIK	TLE	-	-	-	-	643
82	4.13	SCOTT R. & JOAN M. BOLLMAN	PLE, TLE	-	-	-	4,375	2,562
83	4.13 & 4.14	LAKE SIDE REAL ESTATE, LLC.	FEE, TLE	-	5,204	5,204	-	955
84	4.13	BOSKO VUJATOVIC & ALYSSA L. VUJATOVIC	TLE	-	-	-	-	826
85	4.13	STADELMANN ENGINEERING, INC. PROFIT SHARING TRUST	TLE	-	-	-	-	826
86	4.13	APPLE VALLEY GARDENS CONDOMINIUM	TLE	-	-	-	-	2,096
87	4.14	RCL INVESTMENT IV, LLC.	TLE	-	-	-	-	1,931
88	4.14	SAMCIN ENTERPRISES, LLC.	TLE	-	-	-	-	154
89	4.14	MORRIS L. & MARIA C. STOE BNER, AS TRUSTEES OF THE MORRIS L. & MARIA C. STOE BNER REVOCABLE TRUST AGREEMENT DATED SEPTEMBER 10, 1991	TLE	-	-	-	-	838
91	4.14	APPLE VALLEY ASSOCIATES, LLC. AND TW APPLE VALLEY, LLC.	TLE	-	-	-	-	500
92 (3)	4.14	ELIMINATED	-	-	-	-	-	-
93 (2)	4.14	ELIMINATED	-	-	-	-	-	-

4

4

REVISION DATE	_____	_____	_____
7/16/2024 (2)	_____	_____	_____
9/9/2024 (3)	_____	_____	_____

DATE	10/06/2023
GRID FACTOR	N/A



HWY:	MENOMONEE AVE
COUNTY:	WAUKESHA

STATE R/W PROJECT NUMBER:	2720-07-01
CONSTRUCTION PROJECT NUMBER:	2720-07-71

PLAT SHEET	4.03
PS&E SHEET	_____

E

GN

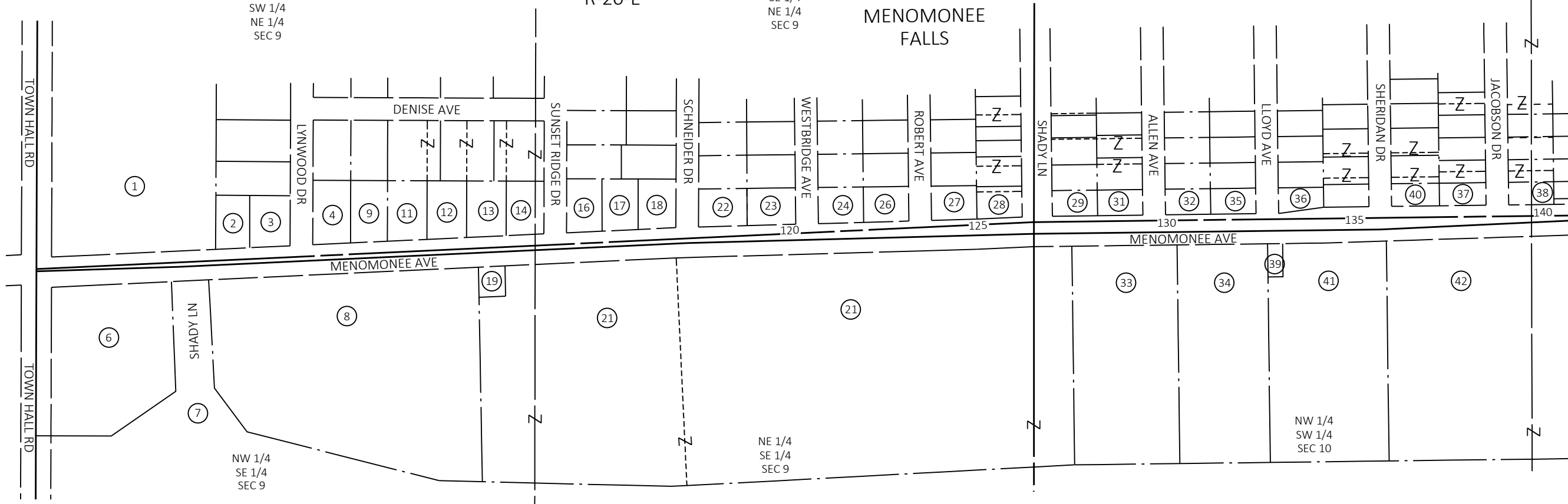


T-8-N
R-20-E

VILLAGE OF
MENOMONEE
FALLS

SE 1/4
NE 1/4
SEC 9

SW 1/4
NE 1/4
SEC 9

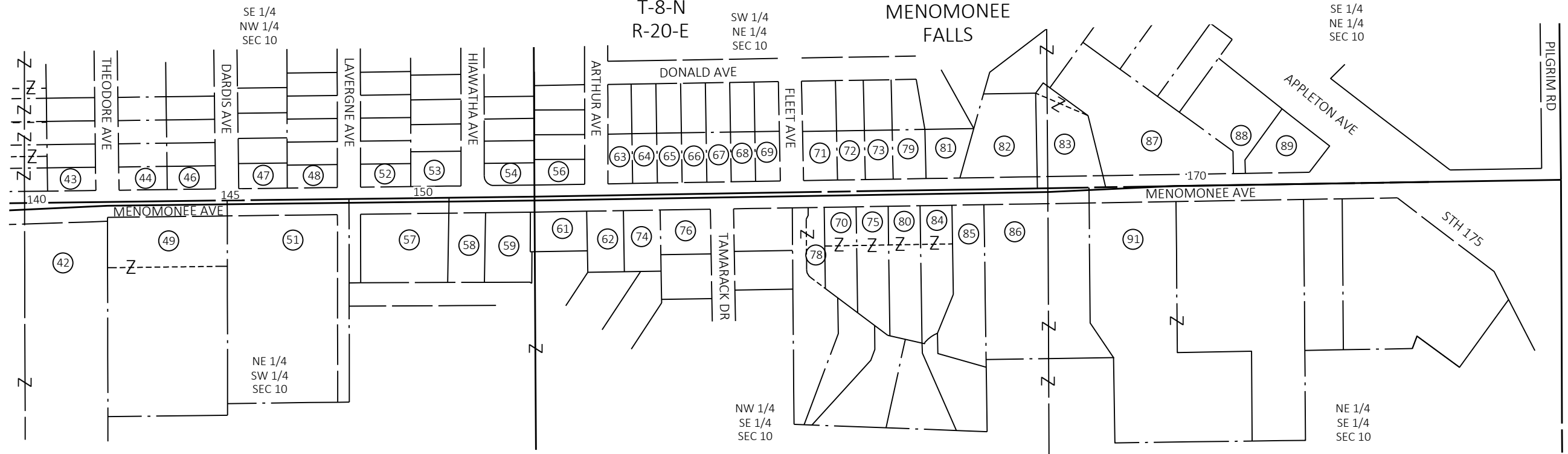


T-8-N
R-20-E

VILLAGE OF
MENOMONEE
FALLS

SW 1/4
NE 1/4
SEC 10

SE 1/4
NW 1/4
SEC 10



4

4

REVISION DATE	_____	_____	_____
7/16/2024 (2)	_____	_____	_____
9/9/2024 (3)	_____	_____	_____

DATE	10/06/2023
GRID FACTOR	0.99999733

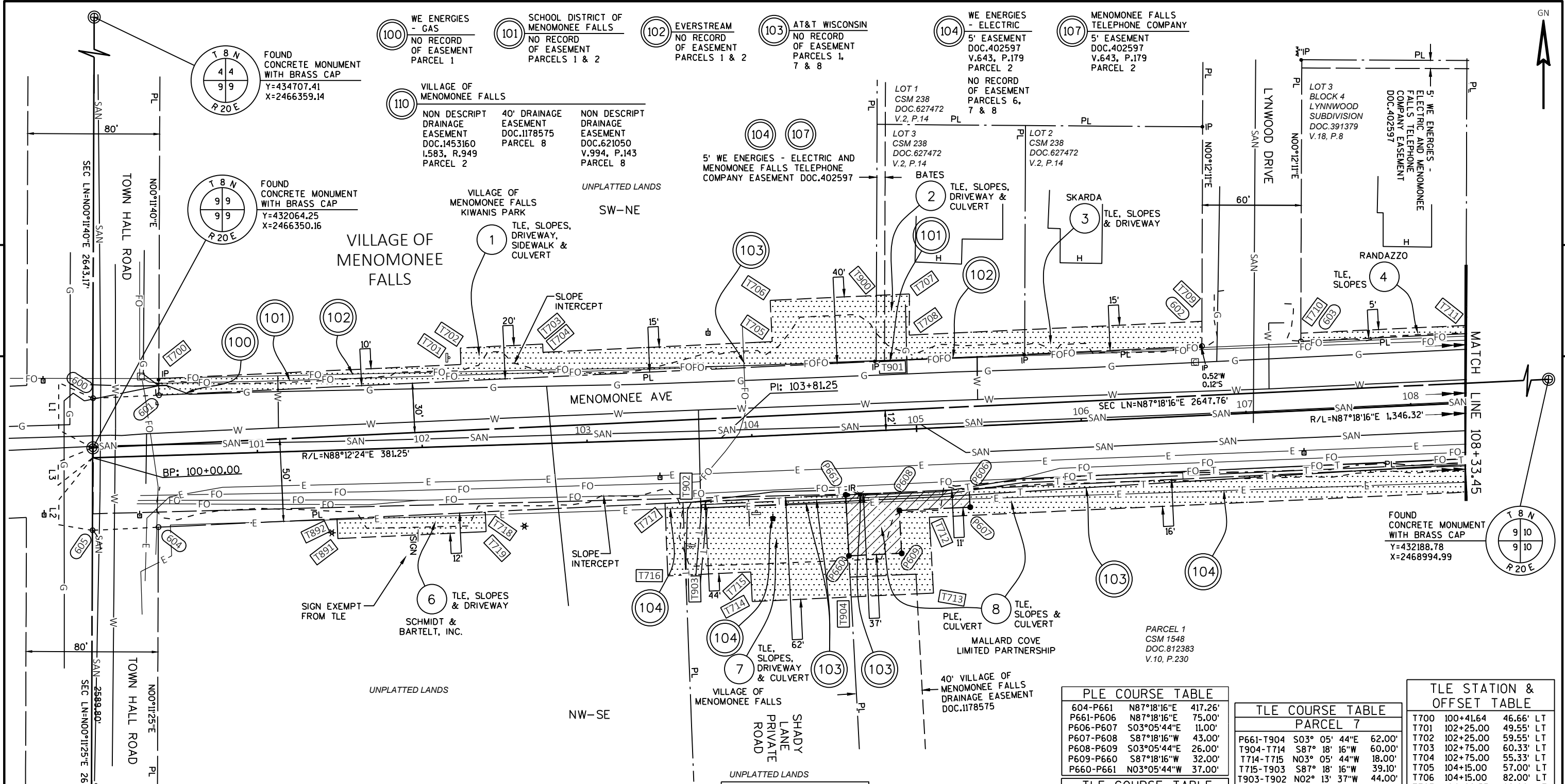


HWY:	MENOMONEE AVE
COUNTY:	WAUKESHA

STATE R/W PROJECT NUMBER:	2720-07-01
CONSTRUCTION PROJECT NUMBER:	2720-07-71

PLAT SHEET	4.04
PS&E SHEET	_____

E



- (100) WE ENERGIES - GAS
NO RECORD OF EASEMENT PARCEL 1
- (101) VILLAGE OF MEMONONEE FALLS
NON DESCRIPT DRAINAGE EASEMENT DOC.1453160 I.583, R.949 PARCEL 2
- (102) SCHOOL DISTRICT OF MEMONONEE FALLS
NO RECORD OF EASEMENT PARCELS 1 & 2
- (103) EVERSTREAM
NO RECORD OF EASEMENT PARCELS 1 & 2
- (104) AT&T WISCONSIN
NO RECORD OF EASEMENT PARCELS 1, 7 & 8
- (107) WE ENERGIES - ELECTRIC
5' EASEMENT DOC.402597 V.643, P.179 PARCEL 2
- (107) MEMONONEE FALLS TELEPHONE COMPANY
5' EASEMENT DOC.402597 V.643, P.179 PARCEL 2

PI/POT	Y	X	DELTA ANGLE
100+00.00	432,058.25	2,466,350.14	
103+81.25	432,070.18	2,466,731.21	00°54'08" LT

HWY	BASIS OF EXISTING R/W	YEAR
MEMONONEE AVE	CSM 238	1964
MEMONONEE AVE	CSM 1548	1972
MEMONONEE AVE	LYNWOOD SUBDIVISION	1954
MEMONONEE AVE	WARRANTY DEED DOCUMENT NO. 1848575	1993
TOWN HALL RD	WARRANTY DEED DOCUMENT NO. 1848575	1993
LYNWOOD DR	CSM 238	1964
LYNWOOD DR	LYNWOOD SUBDIVISION	1954

LINE	BEARING	DISTANCE	POINT
L1	N00°11'40"E	30.04'	
L2	N00°11'25"E	44.06'	
L3	N00°11'25"E	6.00'	

R/W	BEARING	DISTANCE	POINT
600-601	N87°18'16"E	40.05'	
601-602	N87°18'16"E	633.26'	
602-603	N87°18'16"E	60.08'	
603-610	N87°18'16"E	613.61'	
613-604	S87°18'16"W	1,658.32'	
604-605	S87°18'16"W	40.05'	

STATION	OFFSET	POINT
600	100+01.25	36.02' LT
601	100+41.30	36.65' LT
602	106+75.17	42.00' LT
603	107+35.25	42.00' LT
604	100+38.52	43.40' RT
605	99+98.47BK	44.03' RT
P606	105+30.14	38.00' RT
P607	105+30.22	49.00' RT
P608	104+87.22	49.00' RT
P609	104+87.40	75.00' RT
P660	104+55.40	75.00' RT
P661	104+55.14	38.00' RT

TLE	BEARING	DISTANCE	POINT
T901-601	S87° 18' 16"W	435.56'	
601-T700	N00° 11' 40"E	10.01'	
T700-T701	N87° 18' 16"E	183.38'	
T701-T702	N01° 47' 36"W	10.00'	
T702-T703	N87° 18' 16"E	50.01'	
T703-T704	S01° 47' 36"E	5.00'	
T704-T705	N87° 18' 16"E	139.19'	
T705-T706	N02° 41' 44"W	25.00'	
T706-T900	N87° 18' 16"E	64.50'	
T900-T901	S00° 12' 11"W	40.05'	

PLE	BEARING	DISTANCE	POINT
604-P661	N87°18'16"E	417.26'	
P661-P606	N87°18'16"E	75.00'	
P606-P607	S03°05'44"E	11.00'	
P607-P608	S87°18'16"W	43.00'	
P608-P609	S03°05'44"E	26.00'	
P609-P660	S87°18'16"W	32.00'	
P660-P661	N03°05'44"W	37.00'	

TLE	BEARING	DISTANCE	POINT
T902-T903	S02° 13' 37"E	44.00'	
T903-T716	S87° 18' 16"W	10.90'	
T716-T717	N02° 13' 37"W	44.00'	
T717-T902	N87° 18' 16"E	10.90'	

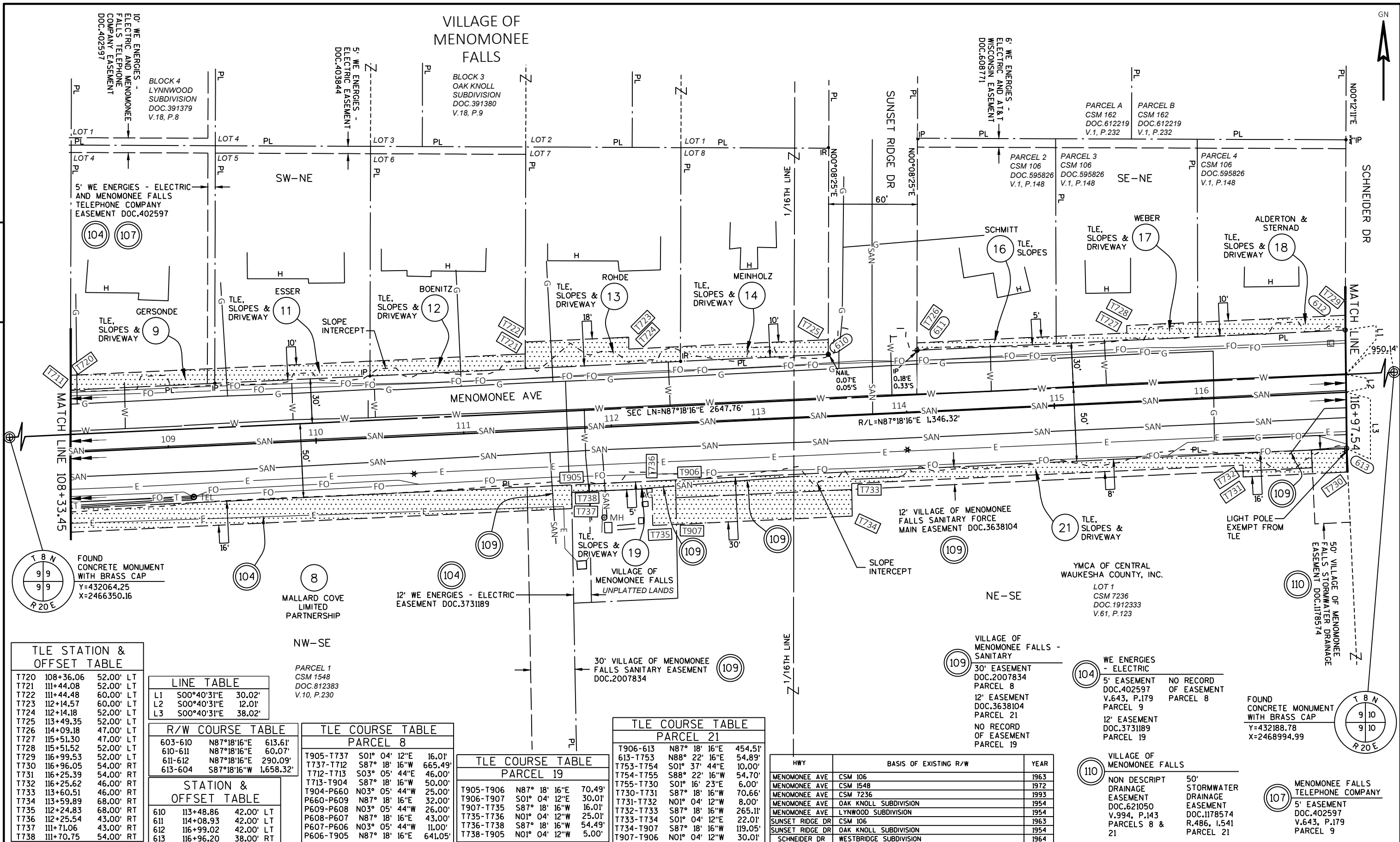
TLE	BEARING	DISTANCE	POINT
T718-T719	S02° 13' 37"E	12.00'	
T719-T891	S87° 18' 16"W	90.00'	
T891-T892	N02° 13' 37"W	12.00'	
T892-T718	N87° 18' 16"E	90.00'	

TLE	BEARING	DISTANCE	POINT
P661-T904	S03° 05' 44"E	62.00'	
T904-T714	S87° 18' 16"W	60.00'	
T714-T715	N03° 05' 44"W	18.00'	
T715-T903	S87° 18' 16"W	39.10'	
T903-T902	N02° 13' 37"W	44.00'	
T902-P661	N87° 18' 16"E	98.77'	

TLE	BEARING	DISTANCE	POINT
T905-T737	S01° 04' 12"E	16.01'	
T737-T712	S87° 18' 16"W	665.49'	
T712-T713	S03° 05' 44"E	46.00'	
T713-T904	S87° 18' 16"W	50.00'	
T904-P660	N03° 05' 44"W	25.00'	
P660-P609	N87° 18' 16"E	32.00'	
P609-P608	N03° 05' 44"W	26.00'	
P608-P607	N87° 18' 16"E	43.00'	
P607-P606	N87° 18' 16"E	11.00'	
P606-T905	N87° 18' 16"E	641.05'	

TLE	STATION	OFFSET	POINT
T700	100+41.64	46.66'	LT
T701	102+25.00	49.55'	LT
T702	102+25.00	59.55'	LT
T703	102+75.00	60.33'	LT
T704	102+75.00	55.33'	LT
T705	104+15.00	57.00'	LT
T706	104+15.00	82.00'	LT
T707	104+99.53	82.00'	LT
T708	104+98.26	57.00'	LT
T709	106+75.93	57.00'	LT
T710	107+35.50	47.00'	LT
T711	108+35.81	47.00'	LT
T712	105+05.26	54.00'	RT
T713	105+05.58	100.00'	RT
T714	103+95.45	100.00'	RT
T715	103+95.45	82.00'	RT
T716	103+46.41	82.56'	RT
T717	103+46.08	38.56'	RT
T718	102+36.98	40.28'	RT
T719	102+37.07	52.28'	RT
T891	101+47.08	53.69'	RT
T892	101+46.99	41.69'	RT

VILLAGE OF MENOMONEE FALLS



TLE STATION & OFFSET TABLE

T720	108+36.06	52.00'	LT
T721	111+44.08	52.00'	LT
T722	111+44.48	60.00'	LT
T723	112+14.57	60.00'	LT
T724	112+14.18	52.00'	LT
T725	113+49.35	52.00'	LT
T726	114+09.18	47.00'	LT
T727	115+51.30	47.00'	LT
T728	115+51.52	52.00'	LT
T729	116+99.53	52.00'	LT
T730	116+96.05	54.00'	RT
T731	116+25.39	54.00'	RT
T732	116+25.62	46.00'	RT
T733	113+60.51	46.00'	RT
T734	113+59.89	68.00'	RT
T735	112+24.83	68.00'	RT
T736	112+25.54	43.00'	RT
T737	111+71.06	43.00'	RT
T738	111+70.75	54.00'	RT

LINE TABLE

L1	S00°40'31"E	30.02'
L2	S00°40'31"E	12.01'
L3	S00°40'31"E	38.02'

R/W COURSE TABLE

603-610	N87°18'16"E	613.61'
610-611	N87°18'16"E	60.07'
611-612	N87°18'16"E	290.09'
613-604	S87°18'16"W	1,658.32'

STATION & OFFSET TABLE

610	113+48.86	42.00'	LT
611	114+08.93	42.00'	LT
612	116+99.02	42.00'	LT
613	116+96.20	38.00'	RT

TLE COURSE TABLE PARCEL 8

T905-T737	S01° 04' 12"E	16.01'
T737-T712	S87° 18' 16"W	665.49'
T712-T713	S03° 05' 44"E	46.00'
T713-T904	S87° 18' 16"W	50.00'
T904-P660	N03° 05' 44"W	25.00'
P660-P609	N87° 18' 16"E	32.00'
P609-P608	N03° 05' 44"W	26.00'
P608-P607	N87° 18' 16"E	43.00'
P607-P606	N03° 05' 44"W	11.00'
P606-T905	N87° 18' 16"E	641.05'

TLE COURSE TABLE PARCEL 19

T905-T906	N87° 18' 16"E	70.49'
T906-T907	S01° 04' 12"E	30.01'
T907-T735	S87° 18' 16"W	16.01'
T735-T736	N01° 04' 12"W	25.01'
T736-T738	S87° 18' 16"W	54.49'
T738-T905	N01° 04' 12"W	5.00'

TLE COURSE TABLE PARCEL 21

T906-613	N87° 18' 16"E	454.51'
613-T753	N88° 22' 16"E	54.89'
T753-T754	S01° 37' 44"E	10.00'
T754-T755	S88° 22' 16"W	54.70'
T755-T730	S01° 16' 23"E	6.00'
T730-T731	S87° 18' 16"W	70.66'
T731-T732	N01° 04' 12"W	8.00'
T732-T733	S87° 18' 16"W	265.11'
T733-T734	S01° 04' 12"E	22.01'
T734-T907	S87° 18' 16"W	119.05'
T907-T906	N01° 04' 12"W	30.01'

HWY	BASIS OF EXISTING R/W	YEAR
MENOMONEE AVE	CSM 106	1963
MENOMONEE AVE	CSM 1548	1972
MENOMONEE AVE	CSM 7236	1993
MENOMONEE AVE	OAK KNOLL SUBDIVISION	1954
MENOMONEE AVE	LYNNWOOD SUBDIVISION	1954
SUNSET RIDGE DR	CSM 106	1963
SUNSET RIDGE DR	OAK KNOLL SUBDIVISION	1954
SCHNEIDER DR	WESTBRIDGE SUBDIVISION	1964

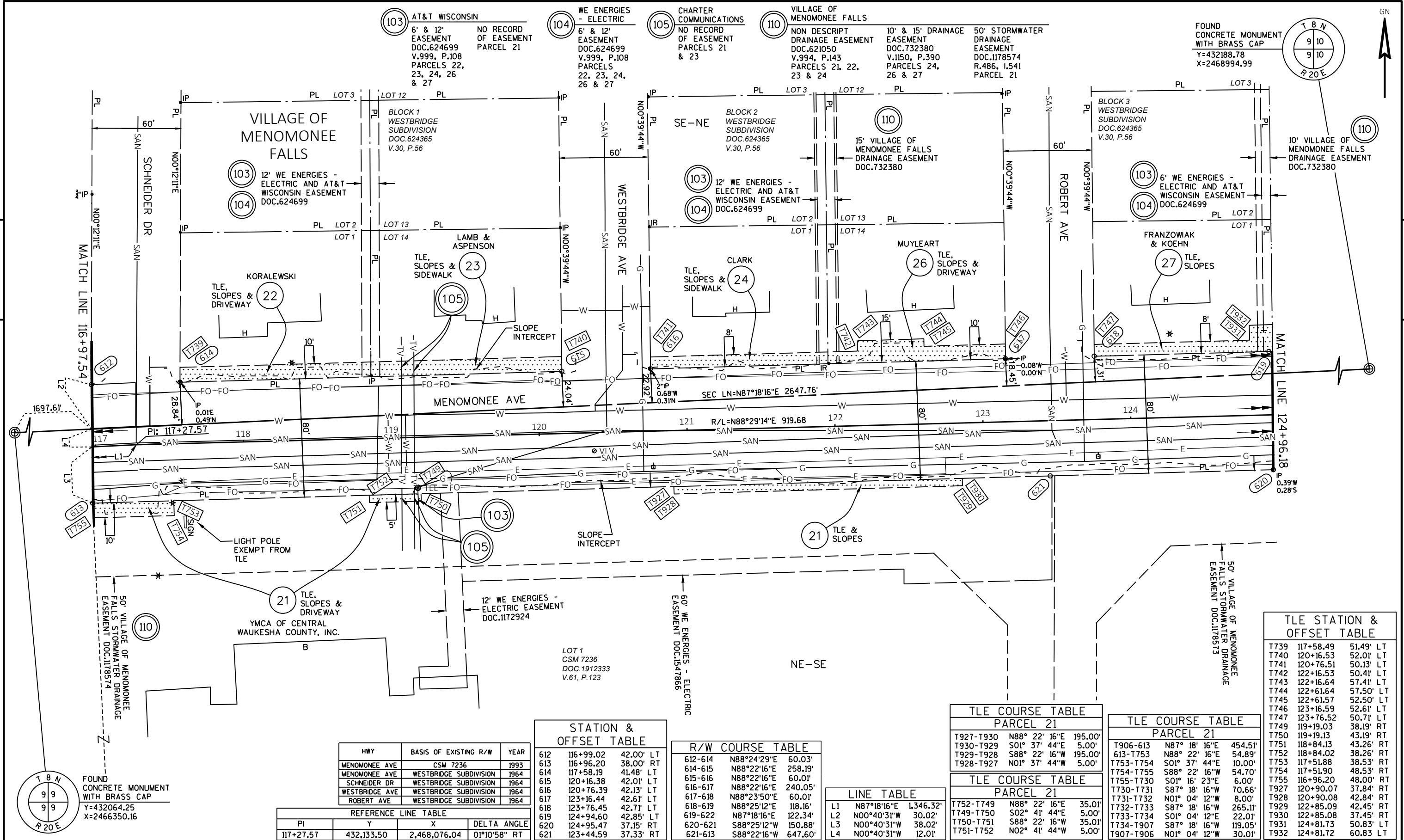
REVISION DATE	7/16/2024 (NC)
9/9/2024 (NC)	
DATE	10/06/2023
GRID FACTOR	0.99999733



HWY: MENOMONEE AVE
COUNTY: WAUKESHA

STATE R/W PROJECT NUMBER: 2720-07-01
CONSTRUCTION PROJECT NUMBER: 2720-07-71

PLAT SHEET 4.06
PS&E SHEET E



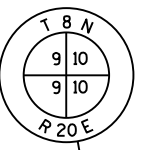
103 AT&T WISCONSIN
6' & 12' NO RECORD
EASEMENT OF EASEMENT
DOC.624699 PARCEL 21
V.999, P.108
PARCELS 22,
23, 24, 26
& 27

104 WE ENERGIES - ELECTRIC
6' & 12' EASEMENT
DOC.624699 V.999, P.108
PARCELS 22, 23, 24,
26 & 27

105 CHARTER COMMUNICATIONS
NO RECORD OF EASEMENT
PARCELS 21 & 23

110 VILLAGE OF MEMOMONEE FALLS
NON DESCRIPT DRAINAGE EASEMENT
DOC.621050 V.994, P.143
PARCELS 21, 22, 23 & 24
10' & 15' DRAINAGE EASEMENT
DOC.732380 V.1150, P.390
PARCELS 24, 26 & 27
50' STORMWATER DRAINAGE EASEMENT
DOC.1178574 R.486, 1.541
PARCEL 21

FOUND CONCRETE MONUMENT WITH BRASS CAP
Y=432188.78
X=2468994.99



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TLE STATION & OFFSET TABLE

T739	117+58.49	51.49'	LT
T740	120+16.53	52.01'	LT
T741	120+76.51	50.13'	LT
T742	122+16.53	50.41'	LT
T743	122+16.64	57.41'	LT
T744	122+61.64	57.50'	LT
T745	122+61.57	52.50'	LT
T746	123+16.59	52.61'	LT
T747	123+76.52	50.71'	LT
T749	119+19.03	38.19'	RT
T750	119+19.13	43.19'	RT
T751	118+84.13	43.26'	RT
T752	118+84.02	38.26'	RT
T753	117+51.88	38.53'	RT
T754	117+51.90	48.53'	RT
T755	116+96.20	48.00'	RT
T927	120+90.07	37.84'	RT
T928	120+90.08	42.84'	RT
T929	122+85.09	42.45'	RT
T930	122+85.08	37.45'	RT
T931	124+81.73	50.83'	LT
T932	124+81.72	60.83'	LT

TLE COURSE TABLE

PARCEL 21

T927-T930	N88° 22' 16"E	195.00'
T930-T929	S01° 37' 44"E	5.00'
T929-T928	S88° 22' 16"W	195.00'
T928-T927	N01° 37' 44"W	5.00'

TLE COURSE TABLE

PARCEL 21

T906-613	N87° 18' 16"E	454.51'
613-T753	N88° 22' 16"E	54.89'
T754-T755	S88° 22' 16"W	54.70'
T755-T730	S01° 16' 23"E	6.00'
T730-T731	S87° 18' 16"W	70.66'
T731-T732	N01° 04' 12"W	8.00'
T732-T733	S87° 18' 16"W	265.11'
T733-T734	S01° 04' 12"E	22.01'
T734-T907	S87° 18' 16"W	119.05'
T907-T906	N01° 04' 12"W	30.01'

TLE COURSE TABLE

PARCEL 21

T752-T749	N88° 22' 16"E	35.01'
T749-T750	S02° 41' 44"E	5.00'
T750-T751	S88° 22' 16"W	35.01'
T751-T752	N02° 41' 44"W	5.00'

LINE TABLE

L1	N87°18'16"E	1,346.32'
L2	N00°40'31"W	30.02'
L3	N00°40'31"W	38.02'
L4	N00°40'31"W	12.01'

STATION & OFFSET TABLE

612	116+99.02	42.00'	LT
613	116+96.20	38.00'	RT
614	117+58.19	41.48'	LT
615	120+16.38	42.01'	LT
616	120+76.39	42.13'	LT
617	123+16.44	42.61'	LT
618	123+76.45	42.71'	LT
619	124+94.60	42.85'	LT
620	124+95.47	37.15'	RT
621	123+44.59	37.33'	RT

R/W COURSE TABLE

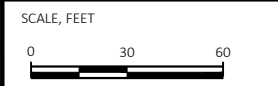
612-614	N88°24'29"E	60.03'
614-615	N88°22'16"E	258.19'
615-616	N88°22'16"E	60.01'
616-617	N88°22'16"E	240.05'
617-618	N88°23'50"E	60.01'
618-619	N88°25'12"E	118.16'
619-622	N87°18'16"E	122.34'
620-621	S88°25'12"W	150.88'
621-613	S88°22'16"W	647.60'

HWY	BASIS OF EXISTING R/W	YEAR
MEMOMONEE AVE	CSM 7236	1993
MEMOMONEE AVE	WESTBRIDGE SUBDIVISION	1964
SCHNEIDER DR	WESTBRIDGE SUBDIVISION	1964
WESTBRIDGE AVE	WESTBRIDGE SUBDIVISION	1964
ROBERT AVE	WESTBRIDGE SUBDIVISION	1964

REFERENCE LINE TABLE			
PI	Y	X	DELTA ANGLE
117+27.57	432,133.50	2,468,076.04	01°10'58" RT

REVISION DATE
7/16/2024 (NC)
9/9/2024 (NC)

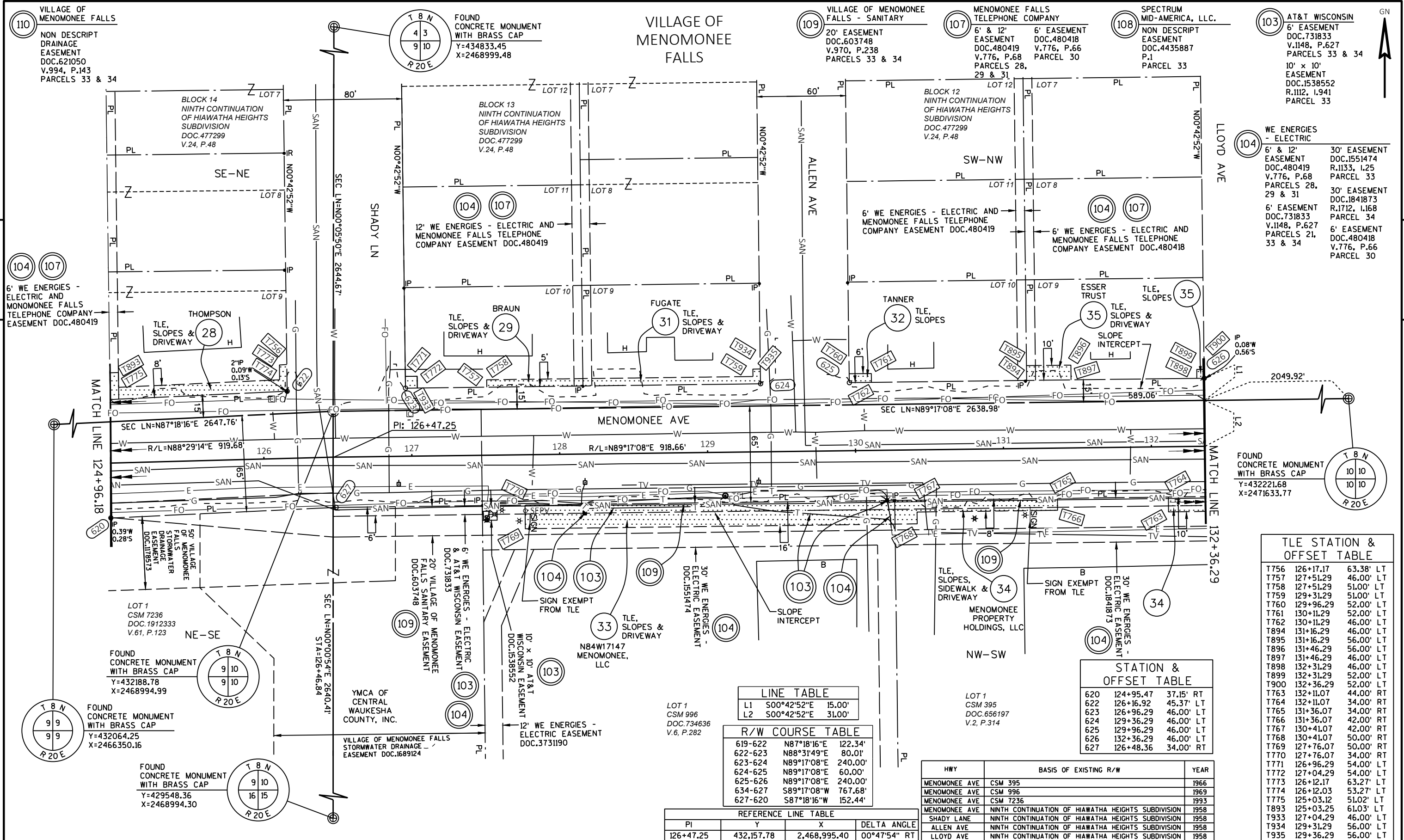
DATE 10/06/2023
GRID FACTOR 0.99999733



HWY: MEMOMONEE AVE
COUNTY: WAUKESHA

STATE R/W PROJECT NUMBER: 2720-07-01
CONSTRUCTION PROJECT NUMBER: 2720-07-71

PLAT SHEET 4.07
PS&E SHEET E



110 VILLAGE OF MENOMONEE FALLS
NON DESCRIPT DRAINAGE EASEMENT DOC.621050 V.994, P.143 PARCELS 33 & 34

109 VILLAGE OF MENOMONEE FALLS - SANITARY
20' EASEMENT DOC.603748 V.970, P.238 PARCELS 33 & 34

107 MENOMONEE FALLS TELEPHONE COMPANY
6' & 12' EASEMENT DOC.480419 V.776, P.68 PARCELS 28, 29 & 31

108 SPECTRUM MID-AMERICA, LLC.
NON DESCRIPT EASEMENT DOC.4435887 P.1 PARCEL 33

103 AT&T WISCONSIN
6' EASEMENT DOC.731833 V.1148, P.627 PARCELS 33 & 34
10' x 10' EASEMENT DOC.1538552 R.1112, L.941 PARCEL 33

104 WE ENERGIES - ELECTRIC
6' & 12' EASEMENT DOC.480419 V.776, P.68 PARCELS 28, 29 & 31
30' EASEMENT DOC.1551474 R.1133, L.25 PARCEL 33
30' EASEMENT DOC.1841873 R.1712, L.168 PARCEL 34
6' EASEMENT DOC.731833 V.1148, P.627 PARCELS 21, 33 & 34
6' EASEMENT DOC.480418 V.776, P.66 PARCEL 30

FOUND CONCRETE MONUMENT WITH BRASS CAP
Y=432221.68
X=2471633.77

Station	Offset	Side
T756	126+17.17	63.38' LT
T757	127+51.29	46.00' LT
T758	127+51.29	51.00' LT
T759	129+31.29	51.00' LT
T760	129+96.29	52.00' LT
T761	130+11.29	52.00' LT
T762	130+11.29	46.00' LT
T894	131+16.29	46.00' LT
T895	131+16.29	56.00' LT
T896	131+46.29	56.00' LT
T897	131+46.29	46.00' LT
T898	132+31.29	46.00' LT
T899	132+31.29	52.00' LT
T900	132+36.29	52.00' LT
T763	132+11.07	44.00' RT
T764	132+11.07	34.00' RT
T765	131+36.07	34.00' RT
T766	131+36.07	42.00' RT
T767	130+41.07	42.00' RT
T768	130+41.07	50.00' RT
T769	127+76.07	50.00' RT
T770	127+76.07	34.00' RT
T771	126+96.29	54.00' LT
T772	127+04.29	54.00' LT
T773	126+12.17	63.27' LT
T774	126+12.03	53.27' LT
T775	125+03.12	51.02' LT
T893	125+03.25	61.03' LT
T933	127+04.29	46.00' LT
T934	129+31.29	56.00' LT
T935	129+36.29	56.00' LT

Station	Offset	Side
620	124+95.47	37.15' RT
622	126+16.92	45.37' LT
623	126+96.29	46.00' LT
624	129+36.29	46.00' LT
625	129+96.29	46.00' LT
626	132+36.29	46.00' LT
627	126+48.36	34.00' RT

Line	Angle	Distance
L1	S00°42'52"E	15.00'
L2	S00°42'52"E	31.00'

Station	Angle	Distance
619-622	N87°18'16"E	122.34'
622-623	N88°31'49"E	80.01'
623-624	N89°17'08"E	240.00'
624-625	N89°17'08"E	60.00'
625-626	N89°17'08"E	240.00'
634-627	S89°17'08"W	767.68'
627-620	S87°18'16"W	152.44'

PI	Y	X	DELTA ANGLE
126+47.25	432,157.78	2,468,995.40	00°47'54" RT

HWY	BASIS OF EXISTING R/W	YEAR
MENOMONEE AVE	CSM 395	1966
MENOMONEE AVE	CSM 996	1969
MENOMONEE AVE	CSM 7236	1993
MENOMONEE AVE	NINTH CONTINUATION OF HIAWATHA HEIGHTS SUBDIVISION	1958
SHADY LANE	NINTH CONTINUATION OF HIAWATHA HEIGHTS SUBDIVISION	1958
ALLEN AVE	NINTH CONTINUATION OF HIAWATHA HEIGHTS SUBDIVISION	1958
LLOYD AVE	NINTH CONTINUATION OF HIAWATHA HEIGHTS SUBDIVISION	1958

REVISION DATE 7/16/2024 (NC) 9/9/2024 (NC)	DATE 10/06/2023	SCALE, FEET 0 30 60	HWY: MENOMONEE AVE	STATE R/W PROJECT NUMBER: 2720-07-01	PLAT SHEET 4.08
FILE NAME: TPP200.DWG	GRID FACTOR 0.99999733	PLOT DATE: 9/9/2024 3:35 PM	COUNTY: WAUKESHA	CONSTRUCTION PROJECT NUMBER: 2720-07-71	PS&E SHEET E



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110 VILLAGE OF MENOMONEE FALLS
 NON DESCRIPT DRAINAGE EASEMENT DOC.621050 V.994, P.143 PARCELS 34, 39, 41 & 42
 NON DESCRIPT DRAINAGE EASEMENT DOC.565163 V.915, P.358 PARCEL 42

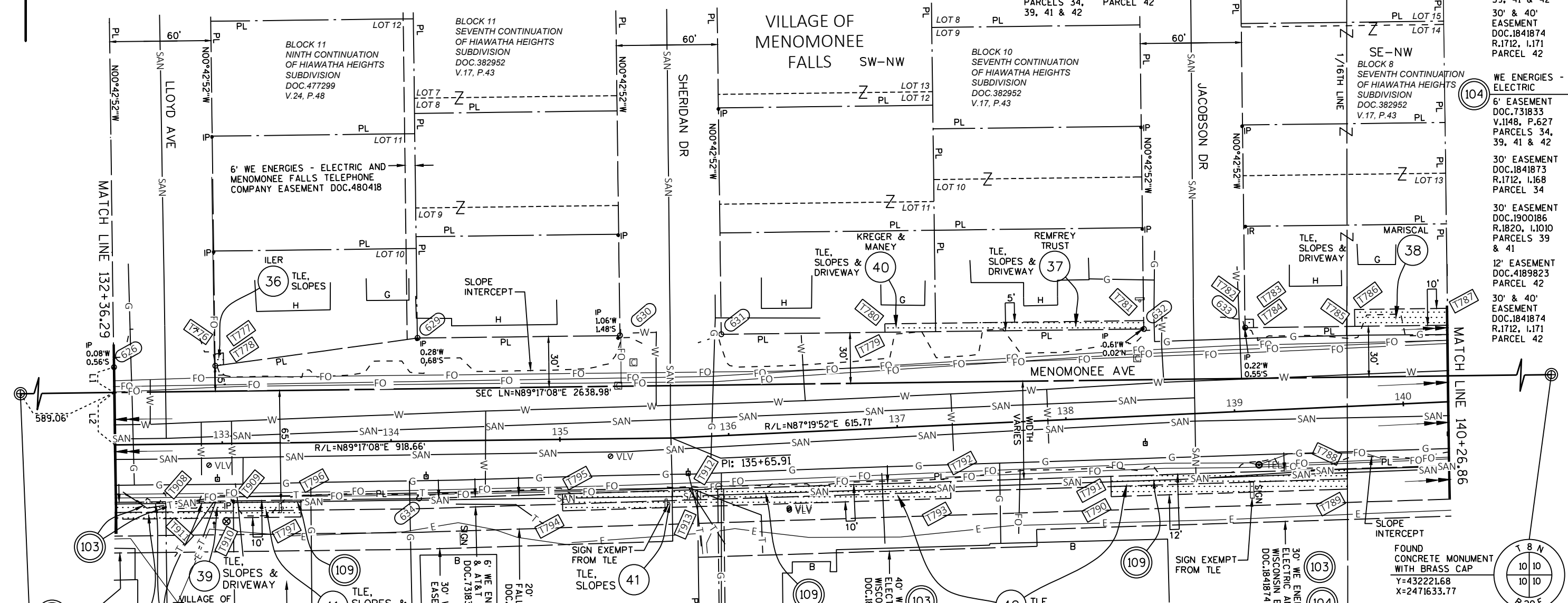
109 VILLAGE OF MENOMONEE FALLS - SANITARY
 20' EASEMENT DOC.603748 V.970, P.238 PARCELS 34, 39, 41 & 42
 15' EASEMENT CSM 6891 DOC.1779484 V.57, P.378 PARCEL 42

103 AT&T WISCONSIN
 6' EASEMENT DOC.731833 V.1148, P.627 PARCELS 34, 39, 41 & 42
 30' & 40' EASEMENT DOC.1841874 R.1712, I.171 PARCEL 42

104 WE ENERGIES - ELECTRIC
 6' EASEMENT DOC.731833 V.1148, P.627 PARCELS 34, 39, 41 & 42
 30' EASEMENT DOC.1841873 R.1712, I.168 PARCEL 34

30' EASEMENT DOC.1900186 R.1820, I.1010 PARCELS 39 & 41
 12' EASEMENT DOC.4189823 PARCEL 42

30' & 40' EASEMENT DOC.1841874 R.1712, I.171 PARCEL 42



PI	Y	X	DELTA ANGLE
135+65.91	432,169.23	2,469,913.99	01°57'16" LT

HWY	BASIS OF EXISTING R/W	YEAR
MENOMONEE AVE	CSM 6891	1992
MENOMONEE AVE	NINTH CONTINUATION OF HIAWATHA HEIGHTS SUBDIVISION	1958
MENOMONEE AVE	SEVENTH CONTINUATION OF HIAWATHA HEIGHTS SUBDIVISION	1953
MENOMONEE AVE	WARRANTY DEED DOCUMENT NO. 4686870	2022
LLOYD AVE	NINTH CONTINUATION OF HIAWATHA HEIGHTS SUBDIVISION	1958
SHERIDAN DR	SEVENTH CONTINUATION OF HIAWATHA HEIGHTS SUBDIVISION	1953
JACOBSON DR	SEVENTH CONTINUATION OF HIAWATHA HEIGHTS SUBDIVISION	1953

STATION	LEFT OFFSET	RIGHT OFFSET
626	132+36.29	46.00' LT
628	132+96.29	46.00' LT
629	134+16.29	61.00' LT
630	135+36.29	61.00' LT
631	135+98.35	59.93' LT
632	138+48.50	51.39' LT
633	139+08.47	49.35' LT
634	134+16.03	34.00' RT

STATION	BEARING	DISTANCE
626-628	N89°17'08"E	60.00'
628-629	N82°09'38"E	120.93'
629-630	N89°17'08"E	120.00'
630-631	N89°17'08"E	60.00'
631-632	N89°17'08"E	250.30'
632-633	N89°17'08"E	60.00'
633-635	N89°17'08"E	245.00'
644-634	S88°09'43"W	764.95'
634-627	S89°17'08"W	767.68'

LINE	BEARING	DISTANCE
L1	N00°42'52"W	15.00'
L2	N00°42'52"W	31.00'

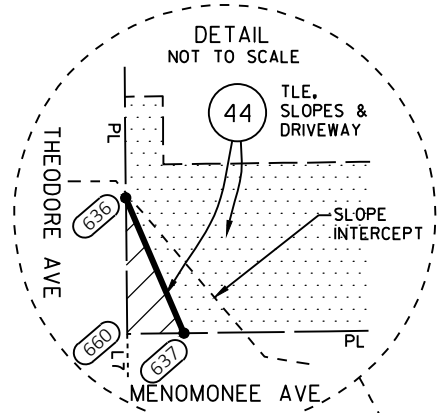
STATION	BEARING	DISTANCE
T909-T796	N89° 17' 08"E	40.00'
T796-T797	S00° 42' 52"E	10.00'
T797-T910	S89° 17' 08"W	40.00'
T910-T909	N00° 42' 52"W	10.00'

STATION	BEARING	DISTANCE
T908-T909	N89° 17' 08"E	40.00'
T909-T910	S00° 42' 52"E	10.00'
T910-T911	S89° 17' 08"W	40.00'
T911-T908	N00° 42' 52"W	10.00'

STATION	BEARING	DISTANCE
T912-T913	S00° 42' 52"E	10.00'
T913-T794	S88° 09' 43"W	80.02'
T794-T795	N00° 42' 52"W	10.00'
T795-T912	N88° 09' 43"E	80.02'

STATION	LEFT OFFSET	RIGHT OFFSET
T776	132+96.29	53.05' LT
T777	133+01.29	53.68' LT
T778	133+01.29	46.62' LT
T779	136+95.29	55.62' LT
T780	136+95.46	61.62' LT
T781	138+48.68	56.39' LT
T782	139+08.64	54.34' LT
T783	139+13.64	54.17' LT
T784	139+13.47	49.17' LT
T785	139+73.43	47.13' LT
T786	139+73.77	57.12' LT
T787	140+28.74	55.25' LT
T788	139+65.04	36.85' RT
T789	139+64.63	48.84' RT
T790	138+24.62	46.81' RT
T791	138+25.03	34.82' RT
T792	137+30.02	33.44' RT
T793	137+29.68	43.43' RT
T794	135+01.07	42.33' RT
T795	135+01.07	32.33' RT
T796	133+46.07	34.00' RT
T797	133+46.07	44.00' RT

REVISION DATE 7/16/2024 (NC) 9/9/2024 (NC)	DATE 10/06/2023	SCALE, FEET 0 30 60	HWY: MENOMONEE AVE	STATE R/W PROJECT NUMBER: 2720-07-01	PLAT SHEET 4.09
FILE NAME: TPP200.DWG	GRID FACTOR 0.99999733	PLOT DATE: 9/7/2024 6:53 AM	COUNTY: WAUKESHA	CONSTRUCTION PROJECT NUMBER: 2720-07-71	PS&E SHEET E



5' & 10' UTILITY EASEMENT DOC.235265 V.293, P.201 PARCELS 43, 44, 46, 47 & 48

VILLAGE OF MENOMONEE FALLS

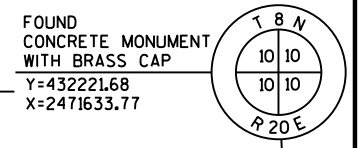
REFERENCE LINE TABLE			
PI	Y	X	DELTA ANGLE
141+81.62	432,197.90	2,470,529.03	01°57'16" RT

109 VILLAGE OF MENOMONEE FALLS - SANITARY 20' & 30' SANITARY OF EASEMENT PARCEL 49 DOC.603748 V.970, P.238 PARCEL 42

110 VILLAGE OF MENOMONEE FALLS NON DESCRIPT DRAINAGE EASEMENT DOC.565163 V.915, P.358 PARCELS 42, 49 & 51

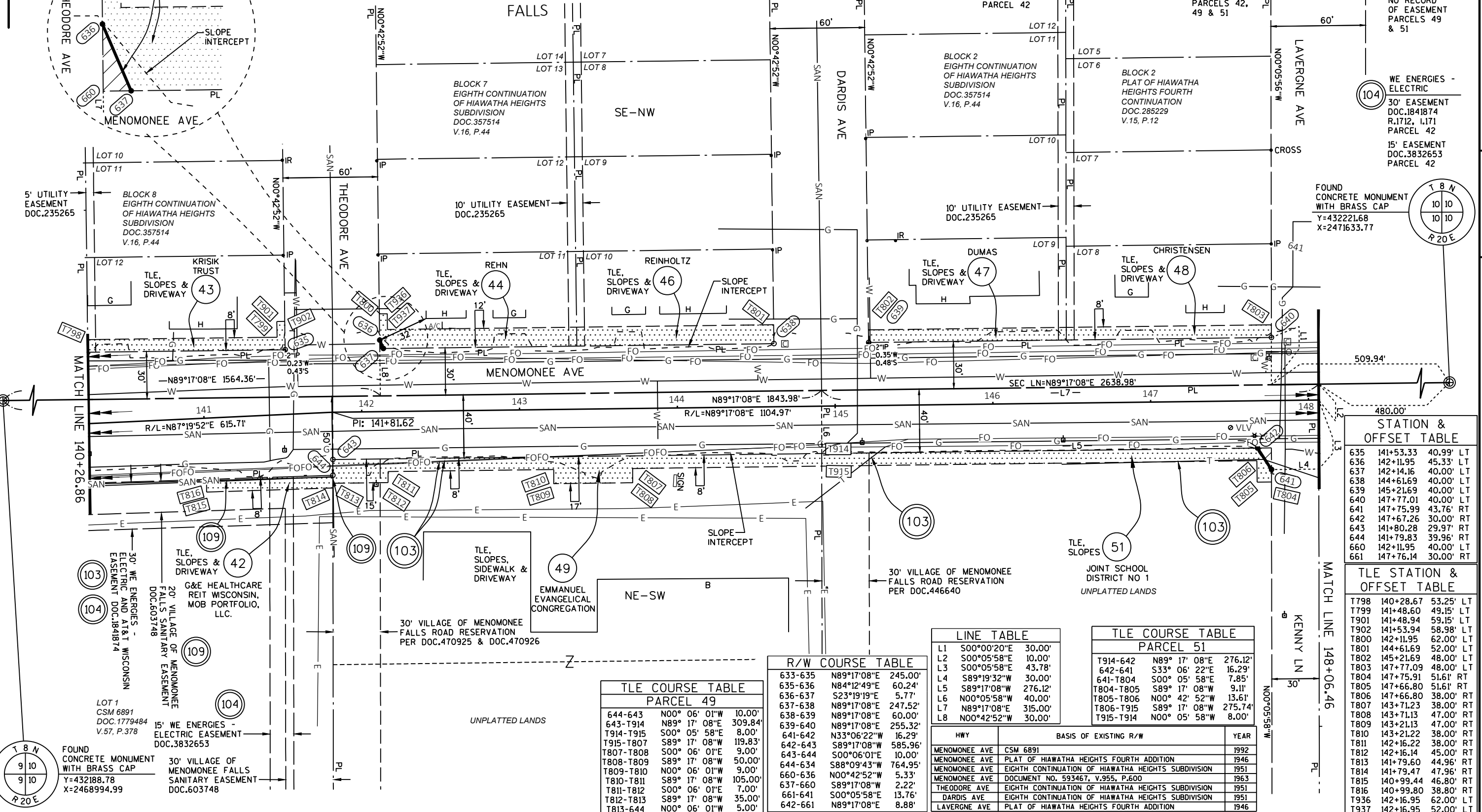
103 AT&T WISCONSIN 30' EASEMENT DOC.1841874 R.1712, I.171 PARCEL 42 NO RECORD OF EASEMENT PARCELS 49 & 51

104 WE ENERGIES - ELECTRIC 30' EASEMENT DOC.1841874 R.1712, I.171 PARCEL 42 15' EASEMENT DOC.3832653 PARCEL 42



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STATION & OFFSET TABLE			
635	141+53.33	40.99'	LT
636	142+11.95	45.33'	LT
637	142+14.16	40.00'	LT
638	144+61.69	40.00'	LT
639	145+21.69	40.00'	LT
640	147+77.01	40.00'	LT
641	147+75.99	43.76'	RT
642	147+67.26	30.00'	RT
643	141+80.28	29.97'	RT
644	141+79.83	39.96'	RT
660	142+11.95	40.00'	LT
661	147+76.14	30.00'	RT

TLE STATION & OFFSET TABLE			
T798	140+28.67	53.25'	LT
T799	141+48.60	49.15'	LT
T901	141+48.94	59.15'	LT
T902	141+53.94	58.98'	LT
T800	142+11.95	62.00'	LT
T801	144+61.69	52.00'	LT
T802	145+21.69	48.00'	LT
T803	147+77.09	48.00'	LT
T804	147+75.99	51.61'	RT
T805	147+66.80	51.61'	RT
T806	147+66.80	38.00'	RT
T807	143+71.23	38.00'	RT
T808	143+71.13	47.00'	RT
T809	143+21.13	47.00'	RT
T810	143+21.22	38.00'	RT
T811	142+16.22	38.00'	RT
T812	142+16.14	45.00'	RT
T813	141+79.60	44.96'	RT
T814	141+79.47	47.96'	RT
T815	140+99.44	46.80'	RT
T816	140+99.80	38.80'	RT
T936	142+16.95	62.00'	LT
T937	142+16.95	52.00'	LT

LINE TABLE		
L1	S00°00'20"E	30.00'
L2	S00°05'58"E	10.00'
L3	S00°05'58"E	43.78'
L4	S89°19'32"W	30.00'
L5	S89°17'08"W	276.12'
L6	N00°05'58"W	40.00'
L7	N89°17'08"E	315.00'
L8	N00°42'52"W	30.00'

TLE COURSE TABLE PARCEL 51			
T914-642	N89° 17' 08"E	276.12'	
642-641	S33° 06' 22"E	16.29'	
641-T804	S00° 05' 58"E	7.85'	
T804-T805	S89° 17' 08"W	9.11'	
T805-T806	N00° 42' 52"W	13.61'	
T806-T915	S89° 17' 08"W	275.74'	
T915-T914	N00° 05' 58"W	8.00'	

R/W COURSE TABLE			
633-635	N89°17'08"E	245.00'	
635-636	N84°12'49"E	60.24'	
636-637	S23°19'19"E	5.77'	
637-638	N89°17'08"E	247.52'	
638-639	N89°17'08"E	60.00'	
639-640	N89°17'08"E	255.32'	
641-642	N33°06'22"W	16.29'	
642-643	S89°17'08"W	585.96'	
643-644	S00°06'01"E	10.00'	
644-634	S88°09'43"W	764.95'	
660-636	N00°42'52"W	5.33'	
637-660	S89°17'08"E	2.22'	
661-641	S00°05'58"E	13.76'	
642-661	N89°17'08"E	8.88'	

TLE COURSE TABLE PARCEL 49			
644-643	N00° 06' 01"W	10.00'	
643-T914	N89° 17' 08"E	309.84'	
T914-T915	S00° 05' 58"E	8.00'	
T915-T807	S89° 17' 08"W	119.83'	
T807-T808	S00° 06' 01"E	9.00'	
T808-T809	S89° 17' 08"W	50.00'	
T809-T810	N00° 06' 01"W	9.00'	
T810-T811	S89° 17' 08"W	105.00'	
T811-T812	S00° 06' 01"E	7.00'	
T812-T813	S89° 17' 08"W	35.00'	
T813-644	N00° 06' 01"W	5.00'	

HWY	BASIS OF EXISTING R/W	YEAR
MENOMONEE AVE	CSM 6891	1992
MENOMONEE AVE	PLAT OF HIAWATHA HEIGHTS FOURTH ADDITION	1946
MENOMONEE AVE	EIGHTH CONTINUATION OF HIAWATHA HEIGHTS SUBDIVISION	1951
MENOMONEE AVE	DOCUMENT NO. 593467, V.955, P.600	1963
THEODORE AVE	EIGHTH CONTINUATION OF HIAWATHA HEIGHTS SUBDIVISION	1951
DARDIS AVE	EIGHTH CONTINUATION OF HIAWATHA HEIGHTS SUBDIVISION	1951
LAVERGNE AVE	PLAT OF HIAWATHA HEIGHTS FOURTH ADDITION	1946

REVISION DATE 7/16/2024 (NC) 9/9/2024 (NC)	DATE 10/06/2023	SCALE, FEET 0 30 60	HWY: MENOMONEE AVE	STATE R/W PROJECT NUMBER: 2720-07-01	PLAT SHEET 4.10
	GRID FACTOR 0.99999733		COUNTY: WAUKESHA	CONSTRUCTION PROJECT NUMBER: 2720-07-71	PS&E SHEET E

HWY	BASIS OF EXISTING R/W	YEAR
MENOMONEE AVE	CSM 4580	1984
MENOMONEE AVE	CONTINUATION OF HIAWATHA HEIGHTS SUBDIVISION	1938
MENOMONEE AVE	SECOND CONTINUATION OF HIAWATHA HEIGHTS SUBDIVISION	1940
MENOMONEE AVE	PLAT OF HIAWATHA HEIGHTS FOURTH ADDITION	1946
MENOMONEE AVE	TAMARACK HILLS SUBDIVISION	1977
LAVERGNE AVE	PLAT OF HIAWATHA HEIGHTS FOURTH ADDITION	1946
HIAWATHA AVE	SECOND CONTINUATION OF HIAWATHA HEIGHTS SUBDIVISION	1940
ARTHUR AVE	CONTINUATION OF HIAWATHA HEIGHTS SUBDIVISION	1938

10' UTILITY EASEMENT
DOC.235265
V.293, P.201
PARCELS
52 & 53

5' UTILITY EASEMENT
DOC.235264
V.293, P.198
PARCEL 54

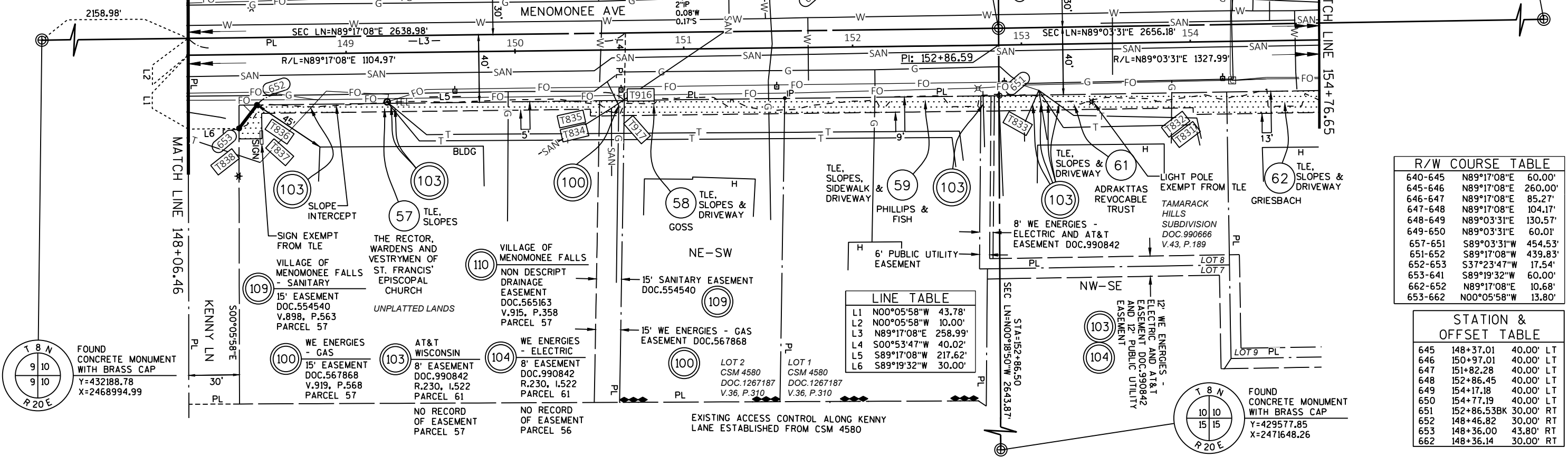
5' UTILITY EASEMENT
DOC.220507
V.275, P.220
PARCEL 56

6' PUBLIC UTILITY EASEMENT
TAMARACK HILLS SUBDIVISION
DOC.990666
V.43, P.189
PARCEL 61

TLE COURSE TABLE PARCEL 57		
653-652	N37° 23' 47"E	17.54'
652-T916	N89° 17' 08"E	217.62'
T916-T917	S00° 53' 47"W	9.00'
T917-T834	S89° 17' 08"W	20.01'
T834-T835	N00° 53' 47"E	4.00'
T835-T836	S89° 17' 08"W	194.74'
T836-T837	S00° 42' 52"E	15.00'
T837-T838	S89° 17' 08"W	13.62'
T838-653	N00° 05' 58"W	6.20'

REFERENCE LINE TABLE			
PI	Y	X	DELTA ANGLE
152+86.59	432,211.68	2,471,633.91	00°13'37" LT

TLE STATION & OFFSET TABLE			
T817	148+37.07	46.00'	LT
T818	149+75.07	46.00'	LT
T819	149+75.01	40.00'	LT
T820	150+77.01	40.00'	LT
T821	150+77.15	53.00'	LT
T822	150+97.10	53.00'	LT
T823	151+97.02	40.00'	LT
T824	151+97.07	45.00'	LT
T825	152+47.07	45.00'	LT
T826	152+47.13	50.00'	LT
T827	152+86.49	50.00'	LT
T828	153+17.33	50.00'	LT
T829	153+17.26	47.00'	LT
T830	154+17.25	47.00'	LT
T831	154+01.06	43.00'	RT
T832	154+01.06	39.00'	RT
T833	152+86.59	39.00'	RT
T834	150+44.17	39.00'	RT
T835	150+44.29	35.00'	RT
T836	148+49.55	35.00'	RT
T837	148+49.55	50.00'	RT
T838	148+35.93	50.00'	RT



FOUND CONCRETE MONUMENT WITH BRASS CAP	
Y=432188.78	X=2468994.99

LINE TABLE	
L1	N00°05'58"W 43.78'
L2	N00°05'58"W 10.00'
L3	N89°17'08"E 258.99'
L4	S00°53'47"W 40.02'
L5	S89°17'08"W 217.62'
L6	S89°19'32"W 30.00'

R/W COURSE TABLE		
640-645	N89°17'08"E	60.00'
645-646	N89°17'08"E	260.00'
646-647	N89°17'08"E	85.27'
647-648	N89°17'08"E	104.17'
648-649	N89°03'31"E	130.57'
649-650	N89°03'31"E	60.01'
657-651	S89°03'31"W	454.53'
651-652	S89°17'08"W	439.83'
652-653	S37°23'47"W	17.54'
653-641	S89°19'32"W	60.00'
662-652	N89°17'08"E	10.68'
653-662	N00°05'58"W	13.80'

STATION & OFFSET TABLE		
645	148+37.01	40.00' LT
646	150+97.01	40.00' LT
647	151+82.28	40.00' LT
648	152+86.45	40.00' LT
649	154+17.18	40.00' LT
650	154+77.19	40.00' LT
651	152+86.53BK	30.00' RT
652	148+46.82	30.00' RT
653	148+36.00	43.80' RT
662	148+36.14	30.00' RT

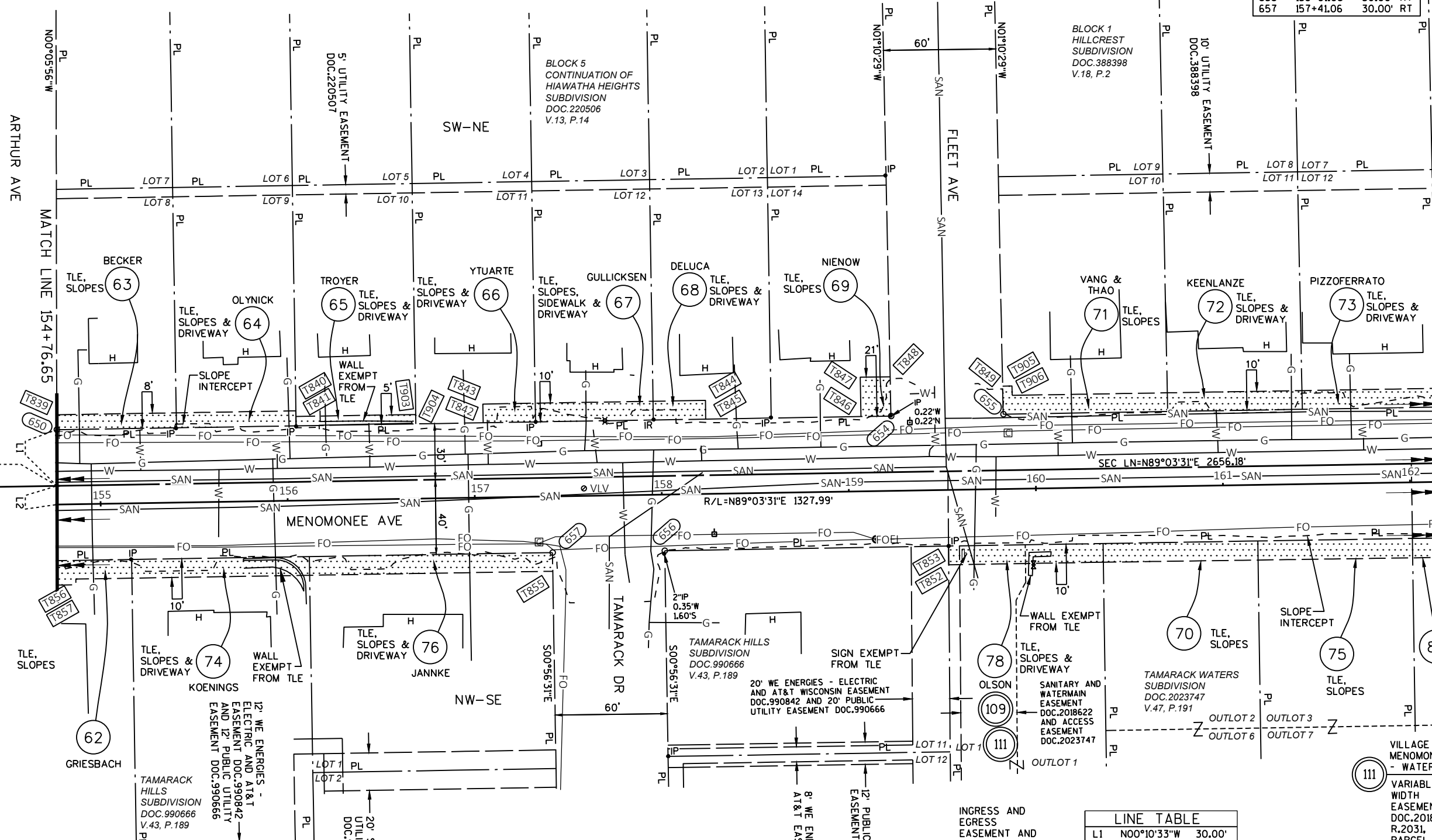
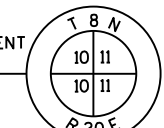
REVISION DATE 7/16/2024 (NC) 9/9/2024 (NC)	DATE 10/06/2023	SCALE, FEET 0 30 60	HWY: MENOMONEE AVE	STATE R/W PROJECT NUMBER: 2720-07-01	PLAT SHEET 4.11
GRID FACTOR 0.99999733			COUNTY: WAUKESHA	CONSTRUCTION PROJECT NUMBER: 2720-07-71	PS&E SHEET

VILLAGE OF
MENOMONEE
FALLS

STATION & OFFSET TABLE			
650	154+77.19	40.00'	LT
654	159+23.17	40.00'	LT
655	159+83.17	40.00'	LT
656	158+01.06	30.00'	RT
657	157+41.06	30.00'	RT

TLE STATION & OFFSET TABLE			
T839	154+77.31	48.00'	LT
T840	156+05.19	48.00'	LT
T841	156+05.19	45.00'	LT
T903	156+69.19	45.00'	LT
T904	156+69.19	40.00'	LT
T842	157+05.14	40.00'	LT
T843	157+05.14	50.00'	LT
T844	158+24.14	50.00'	LT
T845	158+24.14	40.00'	LT
T846	159+07.17	40.00'	LT
T847	159+07.08	61.00'	LT
T848	159+23.08	61.00'	LT
T849	159+83.11	55.00'	LT
T905	159+88.11	55.00'	LT
T906	159+88.13	50.00'	LT
T852	159+52.71	40.00'	RT
T853	159+52.80	30.00'	RT
T855	157+41.06	40.00'	RT
T856	154+86.06	40.00'	RT
T857	154+86.06	43.00'	RT

FOUND
CONCRETE MONUMENT
WITH BRASS CAP
Y=432265.32
X=2474289.59



LINE TABLE		
L1	N00°10'33"W	30.00'
L2	N00°10'33"W	10.00'

R/W COURSE TABLE			
650-654	N89°03'31"E	445.98'	
654-655	N89°03'31"E	60.00'	
655-658	N89°03'31"E	1308.64'	
659-656	S89°03'31"W	1717.25'	
656-657	S89°03'31"W	60.00'	
657-651	S89°03'31"W	454.53'	

HWY	BASIS OF EXISTING R/W	YEAR
MENOMONEE AVE	CONTINUATION OF HIAWATHA HEIGHTS SUBDIVISION	1938
MENOMONEE AVE	HILLCREST SUBDIVISION	1953
MENOMONEE AVE	TAMARACK HILLS SUBDIVISION	1977
MENOMONEE AVE	TAMARACK WATERS SUBDIVISION	1995
ARTHUR AVE	CONTINUATION OF HIAWATHA HEIGHTS SUBDIVISION	1938
FLEET AVE	CONTINUATION OF HIAWATHA HEIGHTS SUBDIVISION	1938
FLEET AVE	HILLCREST SUBDIVISION	1953
TAMARACK DR	TAMARACK HILLS SUBDIVISION	1977

FOUND
CONCRETE MONUMENT
WITH BRASS CAP
Y=432221.68
X=2471633.77

REVISION DATE 7/16/2024 (2) 9/9/2024 (NC)	DATE 10/06/2023	SCALE, FEET 0 30 60	HWY: MENOMONEE AVE	STATE R/W PROJECT NUMBER: 2720-07-01	PLAT SHEET 4.12
	GRID FACTOR 0.99999733		COUNTY: WAUKESHA	CONSTRUCTION PROJECT NUMBER: 2720-07-71	PS&E SHEET

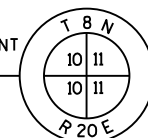
GN

REFERENCE LINE TABLE			
PI	Y	X	DELTA ANGLE
166+14.58	432,233.50	2,472,961.73	01°45'00" LT

LINE TABLE		
L1	S89°03'31"W	176.93'
L2	N00°56'29"W	30.00'
L3	N89°03'31"E	170.04'
L4	S89°03'31"W	117.50'

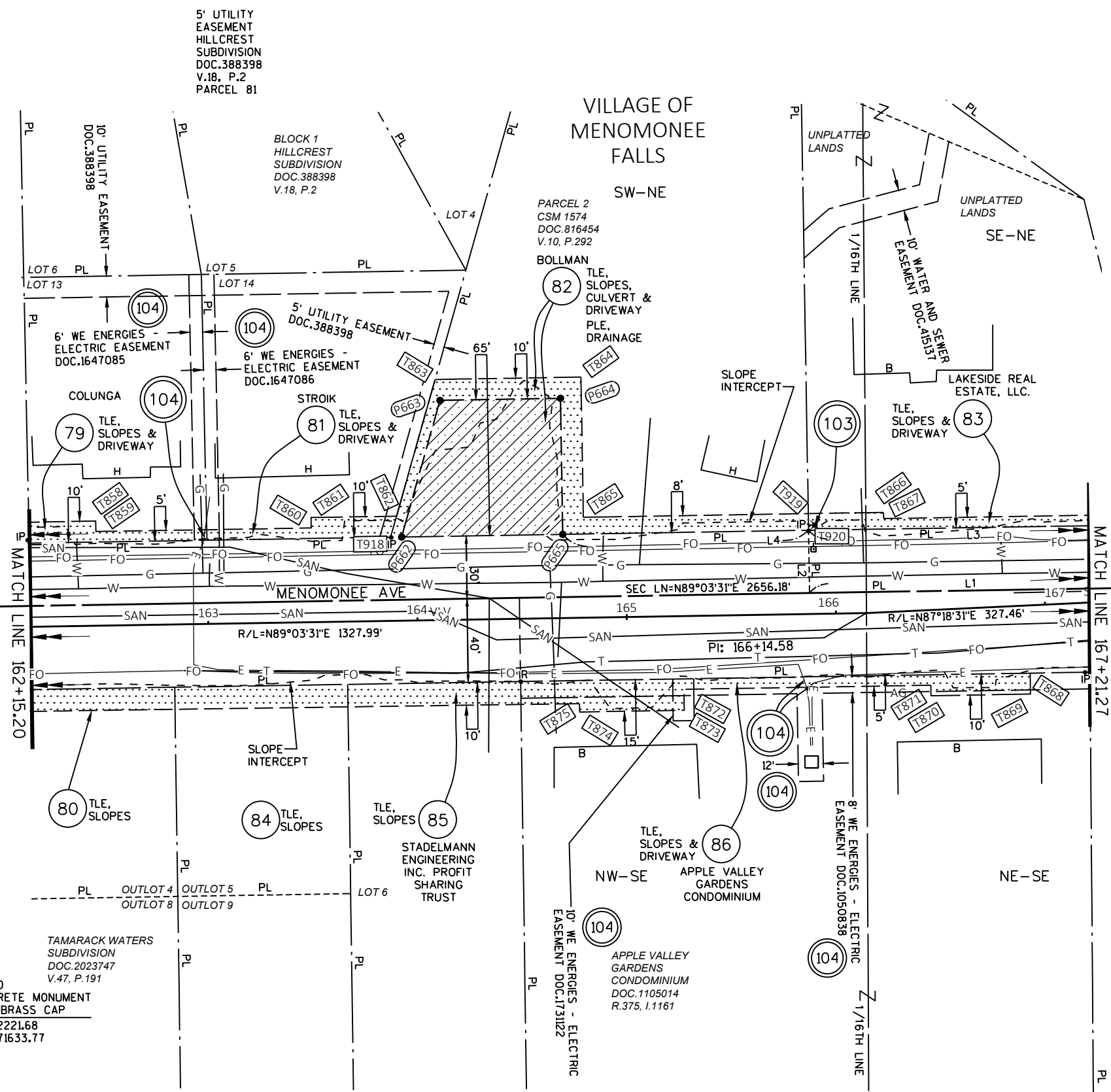
TLE STATION & OFFSET TABLE			
T858	162+47.00	50.00'	LT
T859	162+47.02	45.00'	LT
T860	163+49.97	45.00'	LT
T861	163+49.95	50.00'	LT
T862	163+90.79	50.00'	LT
T863	164+10.25	115.00'	LT
T864	164+80.05	115.00'	LT
T865	164+80.05	48.00'	LT
T866	166+25.00	47.70'	LT
T867	166+25.00	44.70'	LT
T868	166+91.39	32.36'	RT
T869	166+91.09	42.36'	RT
T870	166+43.53	40.90'	RT
T871	166+43.68	35.91'	RT
T872	165+26.78	35.00'	RT
T873	165+26.76	45.00'	RT
T874	164+76.76	45.00'	RT
T875	164+76.77	40.00'	RT

FOUND
CONCRETE MONUMENT
WITH BRASS CAP
Y=432265.32
X=2474289.59



S89°03'31"W 1355.24'

MATCH LINE 167+21.27



STATION & OFFSET TABLE			
P662	163+93.02	40.00'	LT
P663	164+12.48	105.00'	LT
P664	164+70.05	105.00'	LT
P665	164+70.05	40.00'	LT

PLE COURSE TABLE			
P665-P662	S89°03'31"W	77.03'	
P662-P663	N15°43'31"E	67.85'	
P663-P664	N89°03'31"E	57.57'	
P664-P665	S00°56'29"E	65.00'	

R/W COURSE TABLE			
655-658	N89°03'31"E	1308.64'	
659-656	S89°03'31"W	1717.25'	

HWY	BASIS OF EXISTING R/W	YEAR
MENOMONEE AVE	CSM 1574	1972
MENOMONEE AVE	HILLCREST SUBDIVISION	1953
MENOMONEE AVE	TAMARACK WATERS SUBDIVISION	1995
MENOMONEE AVE	APPLE VALLEY GARDENS CONDOMINIUM	1979

TLE COURSE TABLE

PARCEL 82			
T918-T863	N15° 43' 31"E	78.29'	
T863-T864	N89° 03' 31"E	69.80'	
T864-T865	S00° 56' 29"E	67.00'	
T865-T919	N89° 03' 31"E	107.50'	
T919-T920	S00° 56' 29"E	8.00'	
T920-P665	S89° 03' 31"W	117.50'	
P665-P664	N00° 56' 29"W	65.00'	
P664-P663	S89° 03' 31"W	57.57'	
P663-P662	S15° 43' 31"W	67.85'	
P662-T918	S89° 03' 31"W	5.22'	

TLE COURSE TABLE

PARCEL 83			
T921-T920	S89° 03' 31"W	170.04'	
T920-T919	N00° 56' 29"W	8.00'	
T919-T866	N89° 03' 31"E	35.99'	
T866-T867	S02° 41' 29"E	3.00'	
T867-T878	N89° 03' 31"E	132.81'	
T878-T921	S13° 52' 57"E	5.13'	

103 AT&T WISCONSIN NO RECORD OF EASEMENT PARCEL 83

104 WE ENERGIES - ELECTRIC NON DESCRIPT EASEMENT DOC.142665 P.549 PARCELS 79, 81, 82 & 83 6' EASEMENT DOC.1647085 R.1299, 1.578 PARCEL 79 6' EASEMENT DOC.1647086 R.1299, 1.581 PARCEL 81 8' & 12' EASEMENT DOC.1050838 R.304, 1.137 PARCEL 86 10' EASEMENT DOC.1731122 R.1474, 1.1349 PARCEL 86

5' UTILITY EASEMENT HILLCREST SUBDIVISION DOC.388398 V.18, P.2 PARCEL 81

BLOCK 1 HILLCREST SUBDIVISION DOC.388398 V.18, P.2

VILLAGE OF MENOMONEE FALLS SW-NE

PARCEL 2 CSM 1574 DOC.816454 V.10, P.292

BOLLMAN TLE, SLOPES, CULVERT & DRIVEWAY PLE, DRAINAGE

UNPLATTED LANDS

UNPLATTED LANDS

LAKESIDE REAL ESTATE, LLC. TLE, SLOPES & DRIVEWAY

TAMARACK WATERS SUBDIVISION DOC.2023747 V.47, P.191

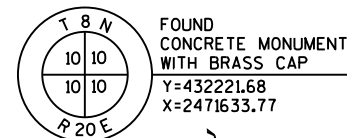
FOUND CONCRETE MONUMENT WITH BRASS CAP Y=432221.68 X=2471633.77

REVISION DATE	DATE	SCALE, FEET
7/16/2024 (NC)	10/06/2023	0 30 60
9/9/2024 (NC)	GRID FACTOR 0.99999733	

HWY: MENOMONEE AVE	STATE R/W PROJECT NUMBER: 2720-07-01
COUNTY: WAUKESHA	CONSTRUCTION PROJECT NUMBER: 2720-07-71

PLAT SHEET 4.13	PS&E SHEET	E
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HWY	BASIS OF EXISTING R/W	YEAR
MENOMONEE AVE	CSM 285	1965
MENOMONEE AVE	CSM 5207	1987
MENOMONEE AVE	CSM 8569	1998
MENOMONEE AVE	WARRANTY DEED DOCUMENT NO. 4125249	2015
STH 175	CSM 8569	1998
STH 175	PROJECT T 0979(13)	1970



109 VILLAGE OF MENOMONEE FALLS - SANITARY
20' EASEMENT DOC.785438 V.1227, P.379 PARCEL 91

111 VILLAGE OF MENOMONEE FALLS - WATERMAIN
20' EASEMENT DOC.785438 V.1227, P.379 PARCEL 91

VILLAGE OF MENOMONEE FALLS

EXISTING ACCESS CONTROL ALONG STH 175 AND MENOMONEE AVE ESTABLISHED PER DOC.895858, R.97, I.920

STATION & OFFSET TABLE			
658	172+91.96	30.00'	LT
659	175+18.46	40.00'	RT

REFERENCE LINE TABLE			
PI	Y	X	DELTA ANGLE
169+42.04	432,248.88	2,473,288.82	01°45'00" RT
175+18.46	432,258.35	2,473,865.16	

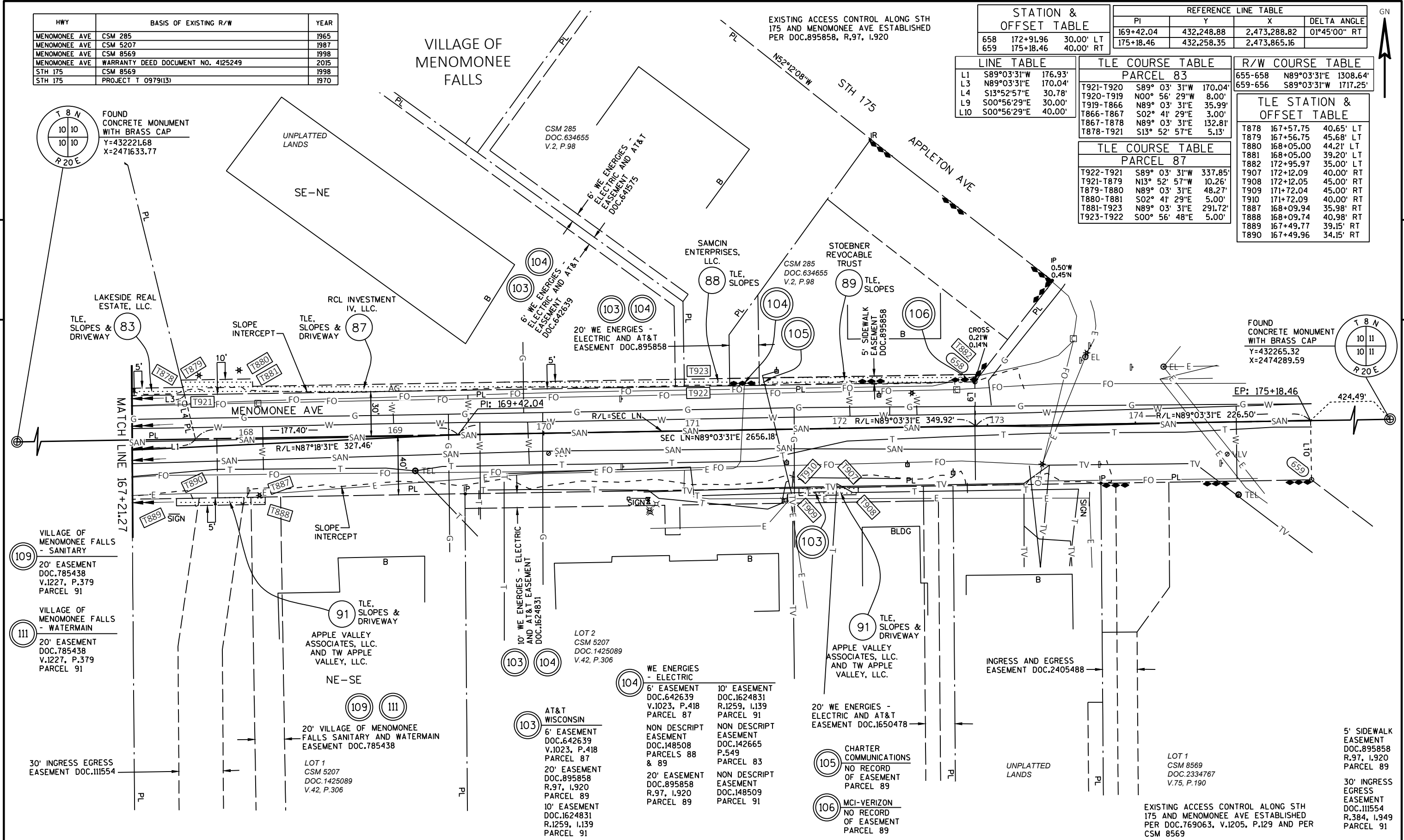
LINE TABLE		
L1	S89°03'31"W	176.93'
L3	N89°03'31"E	170.04'
L4	S13°52'57"E	30.78'
L9	S00°56'29"E	30.00'
L10	S00°56'29"E	40.00'

TLE COURSE TABLE PARCEL 83			
T921-T920	S89° 03' 31"W	170.04'	
T920-T919	N00° 56' 29"W	8.00'	
T919-T866	N89° 03' 31"E	35.99'	
T866-T867	S02° 41' 29"E	3.00'	
T867-T878	N89° 03' 31"E	132.81'	
T878-T921	S13° 52' 57"E	5.13'	

R/W COURSE TABLE			
655-658	N89°03'31"E	1308.64'	
659-656	S89°03'31"W	1717.25'	

TLE COURSE TABLE PARCEL 87			
T922-T921	S89° 03' 31"W	337.85'	
T921-T879	N13° 52' 57"W	10.26'	
T879-T880	N89° 03' 31"E	48.27'	
T880-T881	S02° 41' 29"E	5.00'	
T881-T923	N89° 03' 31"E	291.72'	
T923-T922	S00° 56' 48"E	5.00'	

TLE STATION & OFFSET TABLE		
T878	167+57.75	40.65' LT
T879	167+56.75	45.68' LT
T880	168+05.00	44.21' LT
T881	168+05.00	39.20' LT
T882	172+95.97	35.00' LT
T907	172+12.09	40.00' RT
T908	172+12.05	45.00' RT
T909	171+72.04	45.00' RT
T910	171+72.09	40.00' RT
T887	168+09.94	35.98' RT
T888	168+09.74	40.98' RT
T889	167+49.77	39.15' RT
T890	167+49.96	34.15' RT

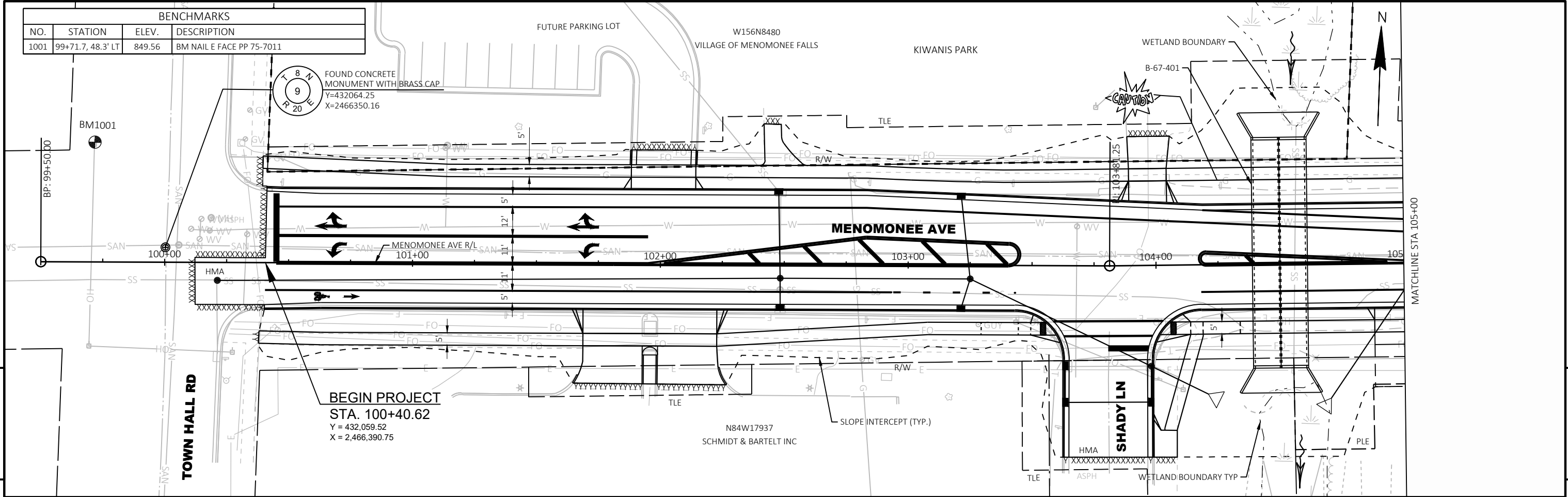


REVISION DATE	DATE	SCALE, FEET
7/16/2024 (2)	10/06/2023	0 30 60
9/9/2024 (3)	GRID FACTOR 0.9999733	

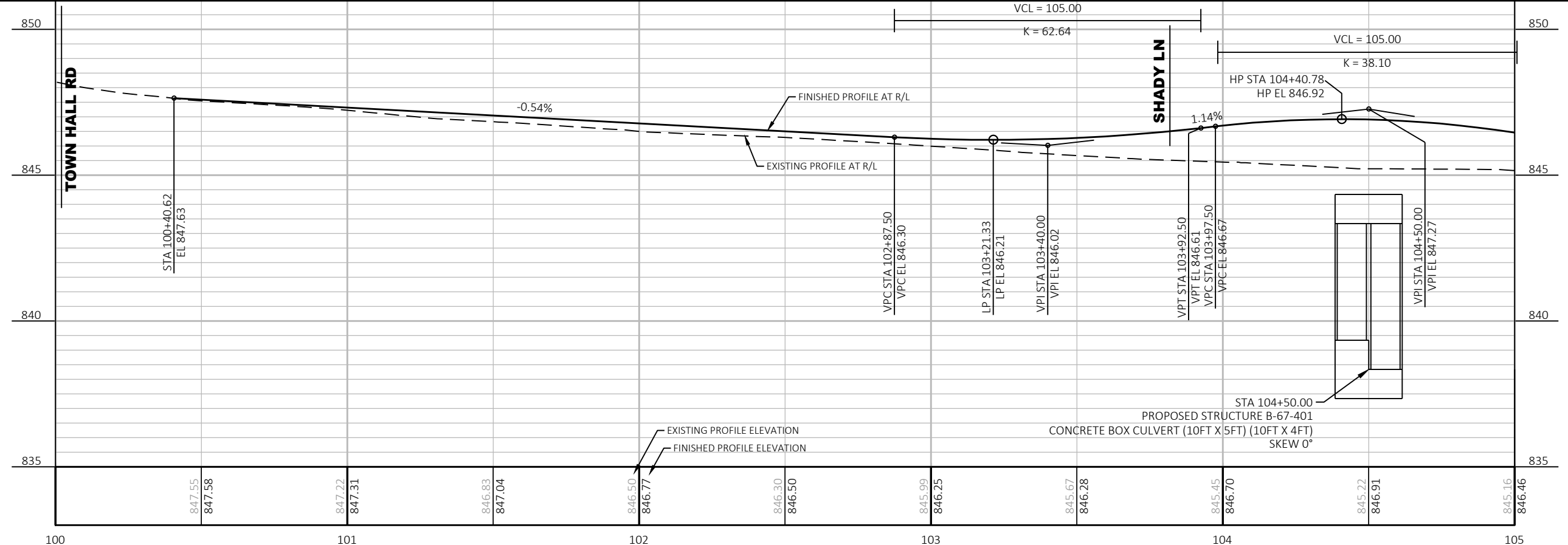
HWY: MENOMONEE AVE	STATE R/W PROJECT NUMBER: 2720-07-01	PLAT SHEET 4.14
COUNTY: WAUKESHA	CONSTRUCTION PROJECT NUMBER: 2720-07-71	PS&E SHEET

FILE NAME: TPP300.DWG	PLOT DATE: 9/7/2024 6:23 AM	PLOT BY: WEINREIS, CHRIS	PLOT NAME:	PLOT SCALE: NO SCALE	WISDOT/CADD SHEET 75
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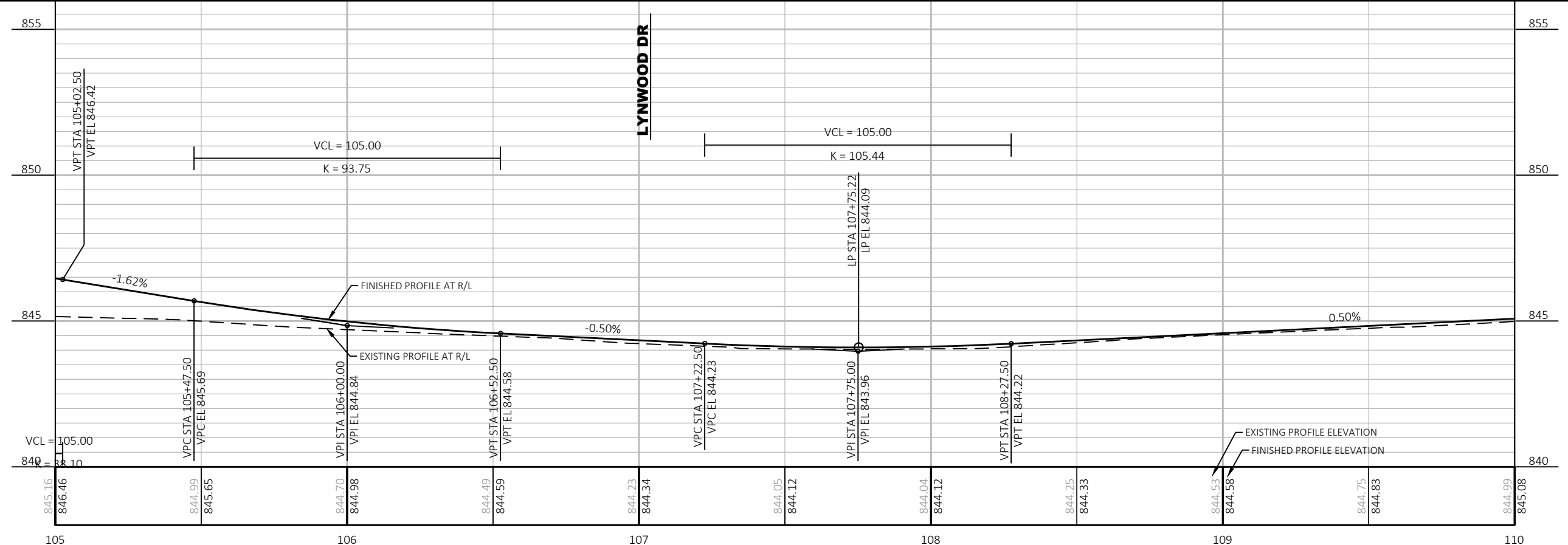
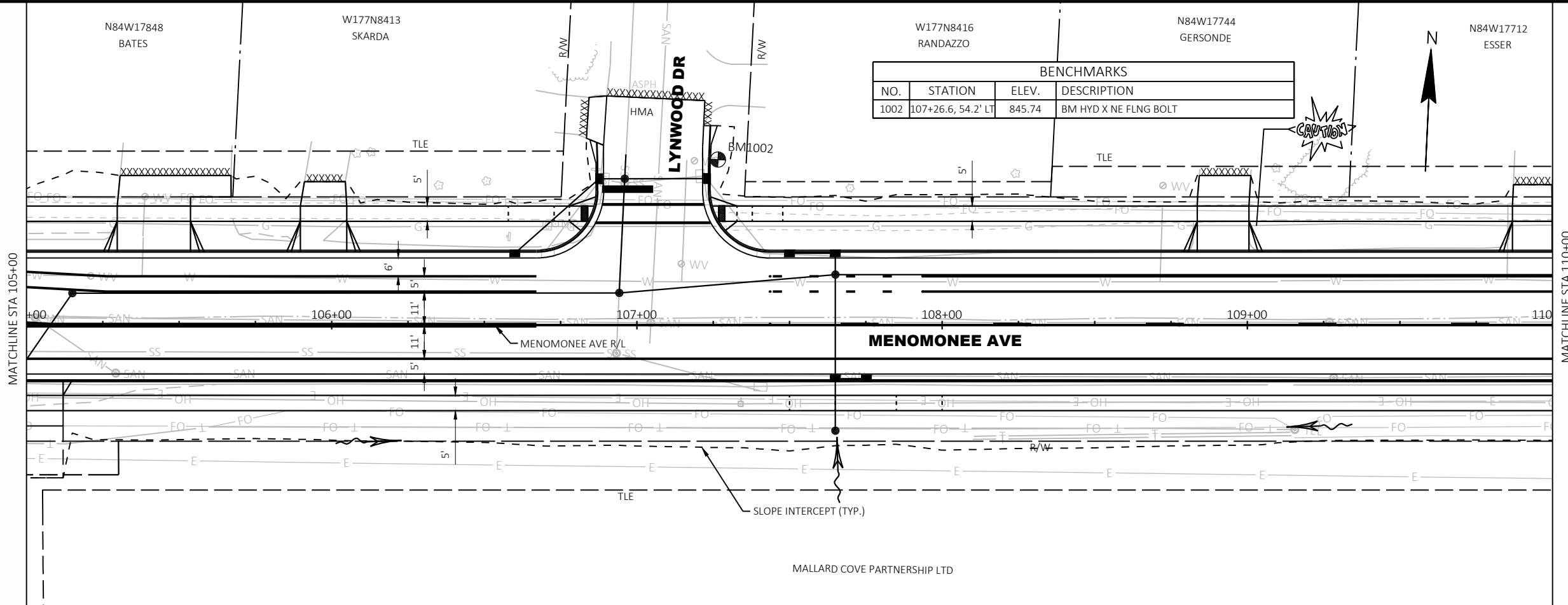
BENCHMARKS			
NO.	STATION	ELEV.	DESCRIPTION
1001	99+71.7, 48.3' LT	849.56	BM NAIL E FACE PP 75-7011



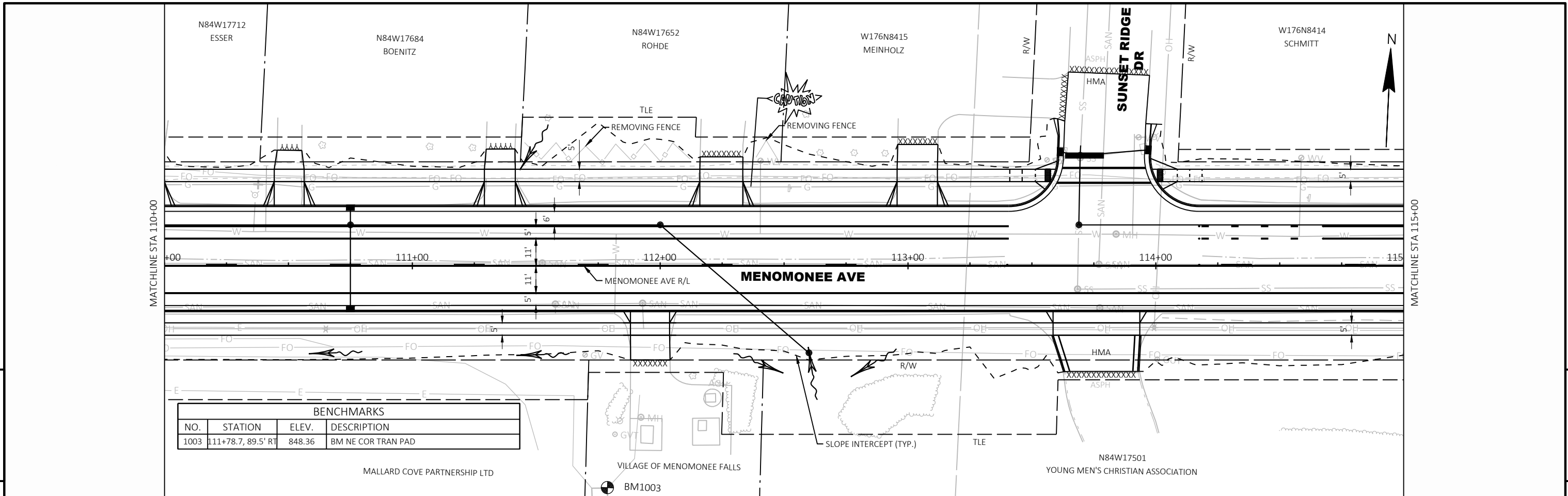
BEGIN PROJECT
STA. 100+40.62
 Y = 432,059.52
 X = 2,466,390.75



PROJECT NO: 2720-07-1	HWY: MENOMONEE AVE	COUNTY: WAUKESHA	PLAN AND PROFILE: MENOMONEE AVENUE	SHEET	E
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PROJECT NO: 2720-07-11 HWY: MENOMONEE AVE COUNTY: WAUKESHA PLAN AND PROFILE: MENOMONEE AVENUE SHEET: 5

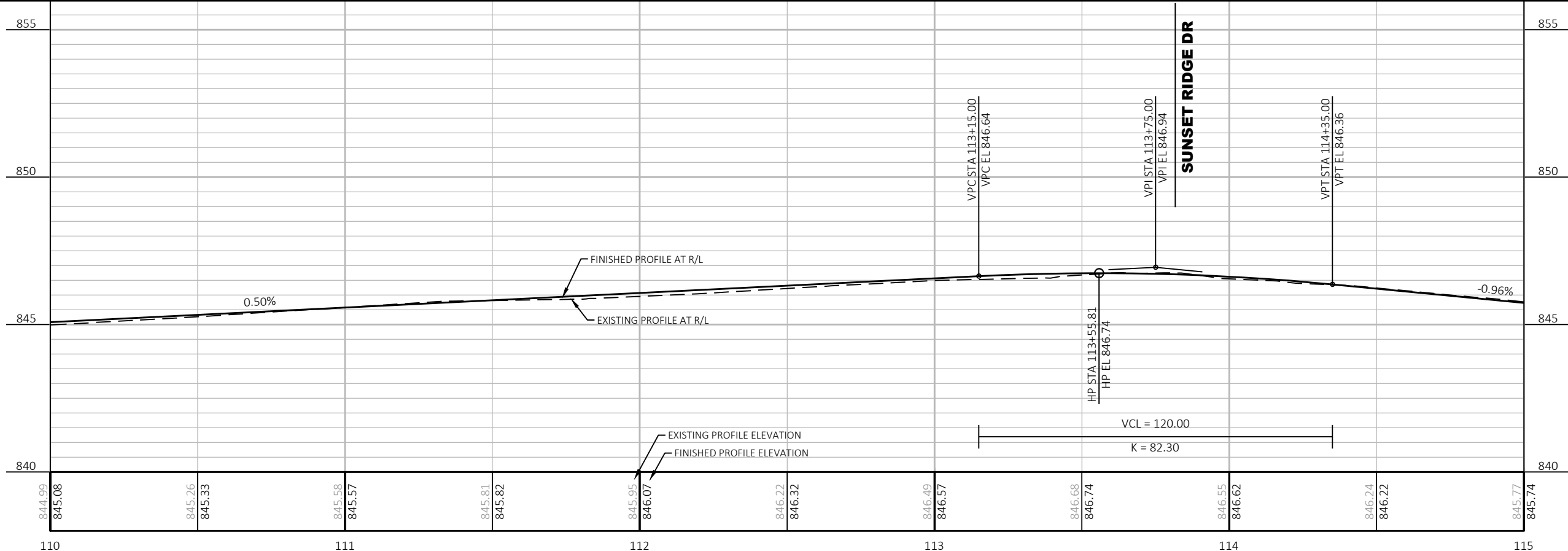


BENCHMARKS			
NO.	STATION	ELEV.	DESCRIPTION
1003	111+78.7, 89.5' RT	848.36	BM NE COR TRAN PAD

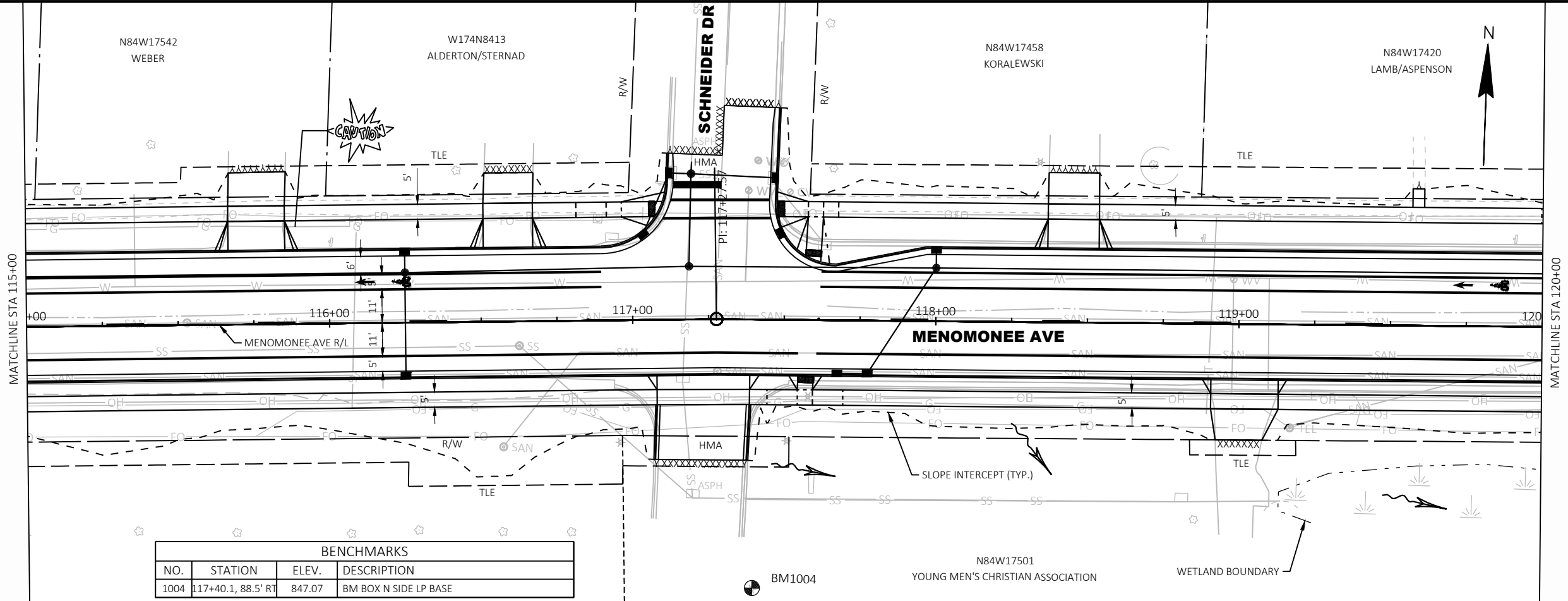
MALLARD COVE PARTNERSHIP LTD

VILLAGE OF MENOMONEE FALLS

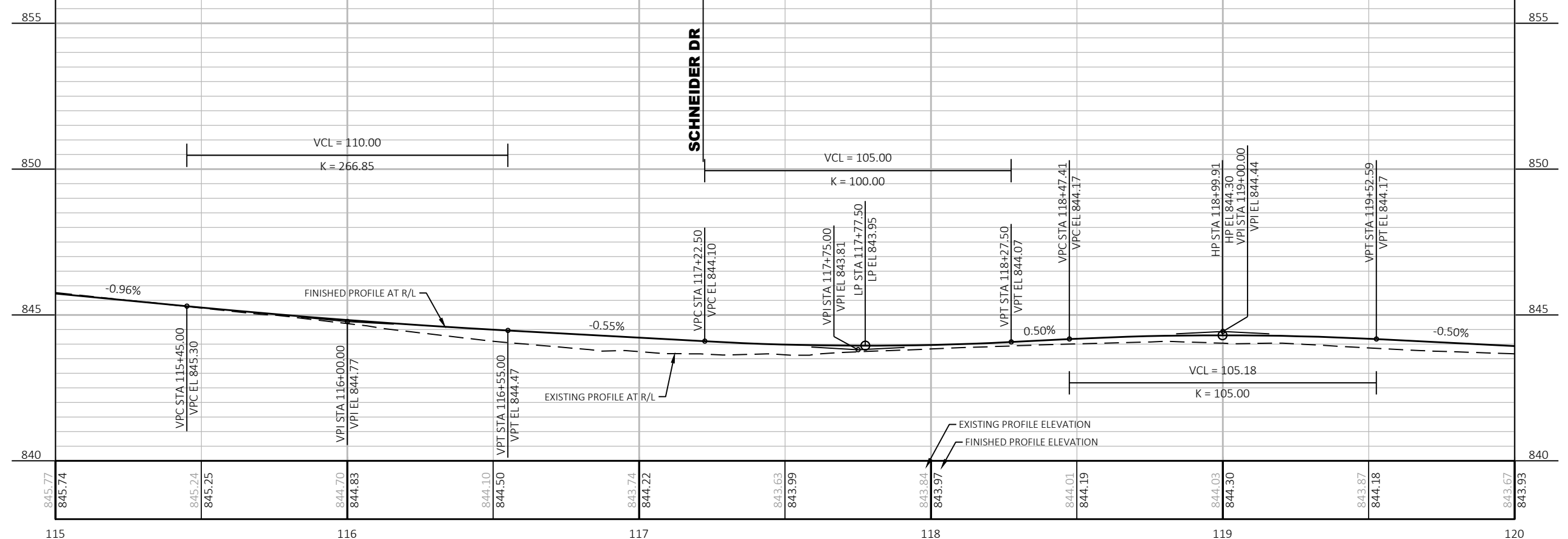
N84W17501
YOUNG MEN'S CHRISTIAN ASSOCIATION



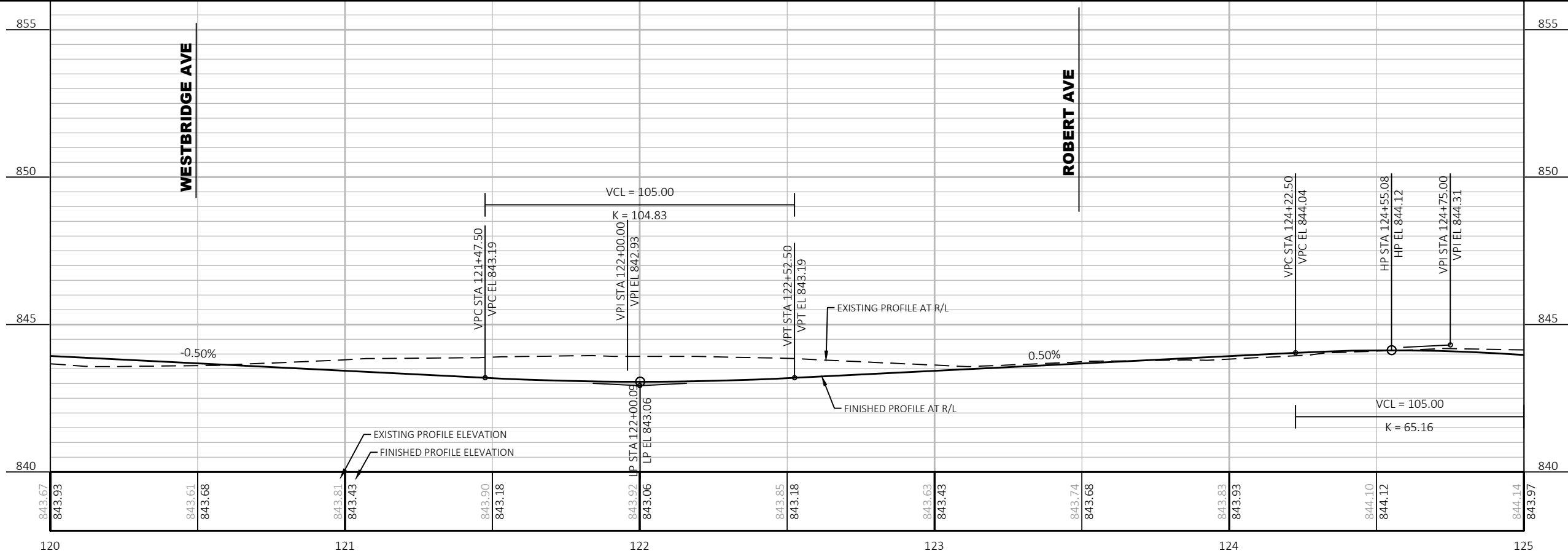
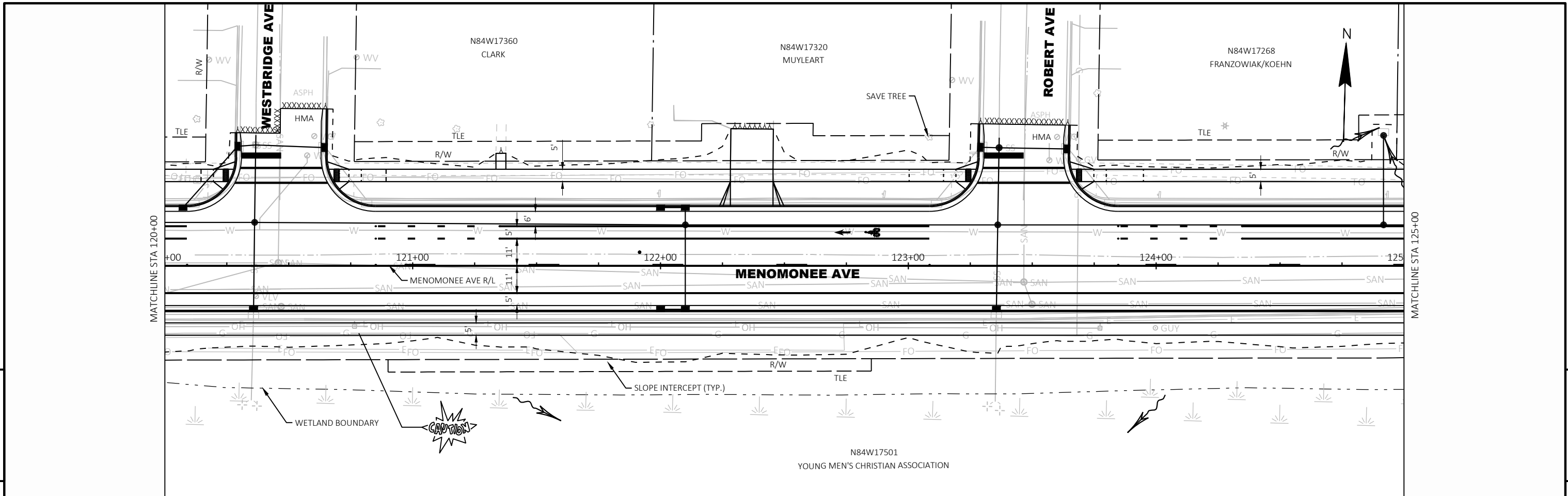
PROJECT NO: 2720-07-11 HWY: MENOMONEE AVE COUNTY: WAUKESHA PLAN AND PROFILE: MENOMONEE AVENUE SHEET 5



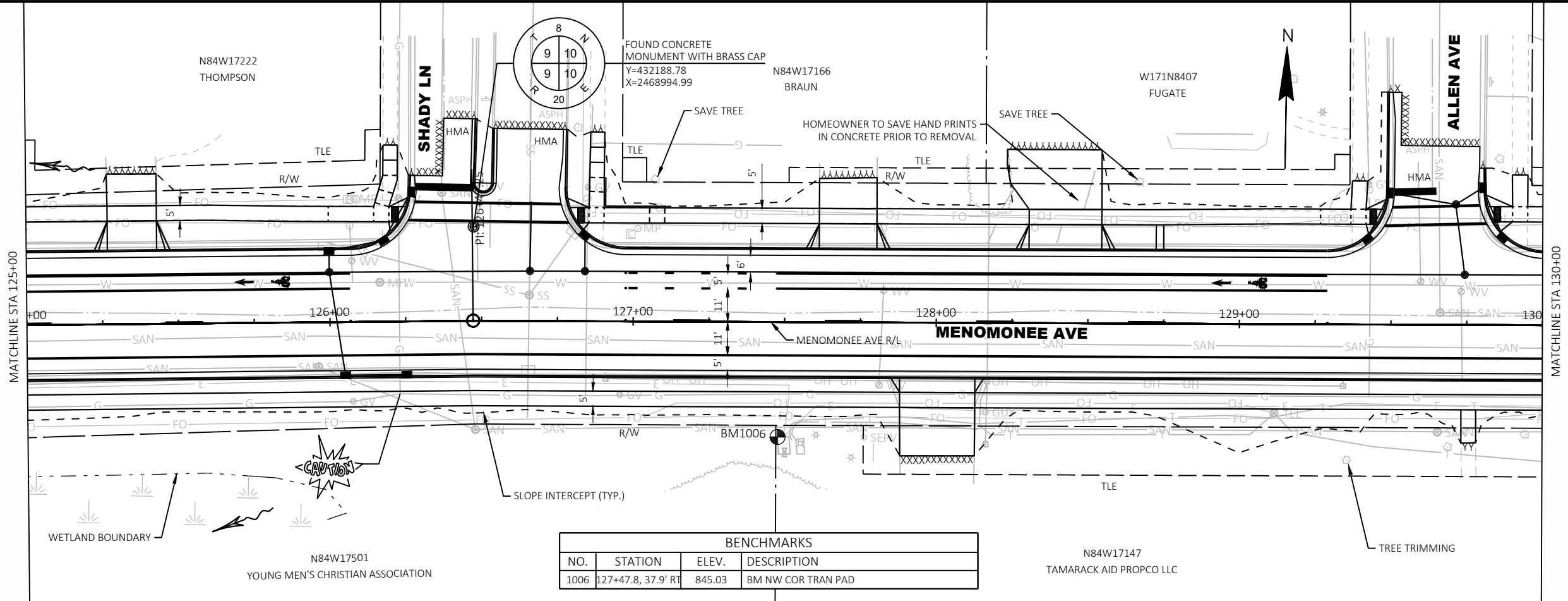
BENCHMARKS			
NO.	STATION	ELEV.	DESCRIPTION
1004	117+40.1, 88.5' RT	847.07	BM BOX N SIDE LP BASE



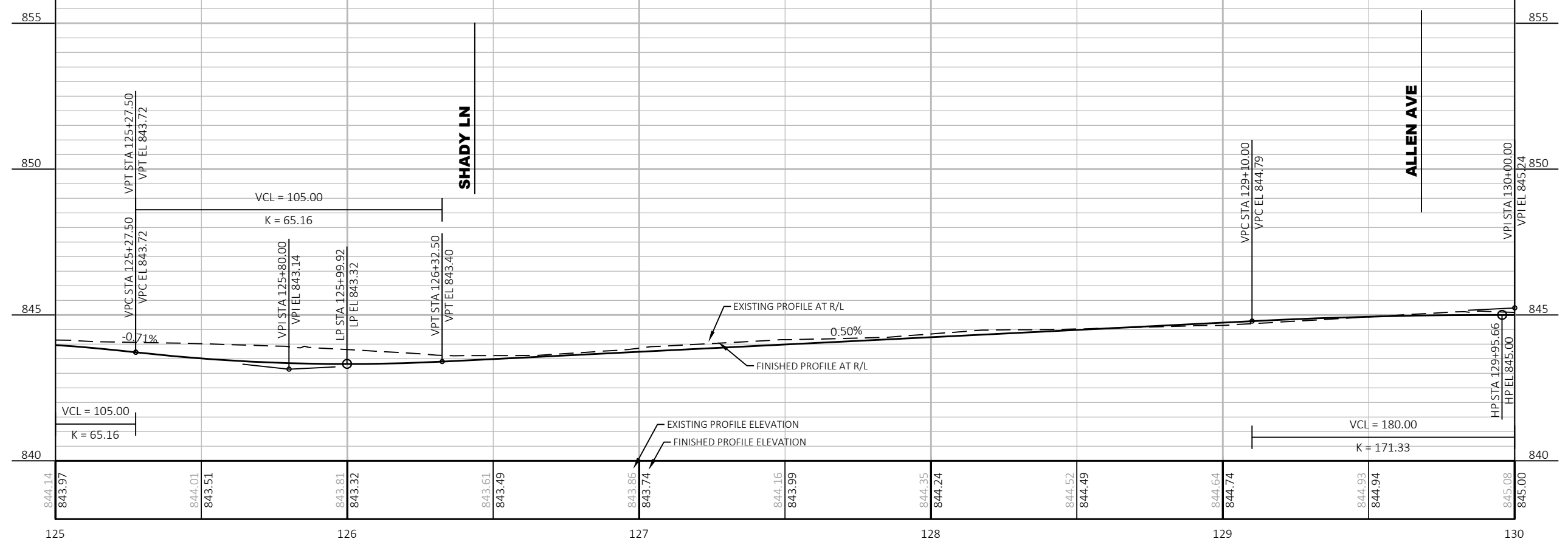
PROJECT NO: 2720-07-14 HWY: MENOMONEE AVE COUNTY: WAUKESHA PLAN AND PROFILE: MENOMONEE AVENUE SHEET 5

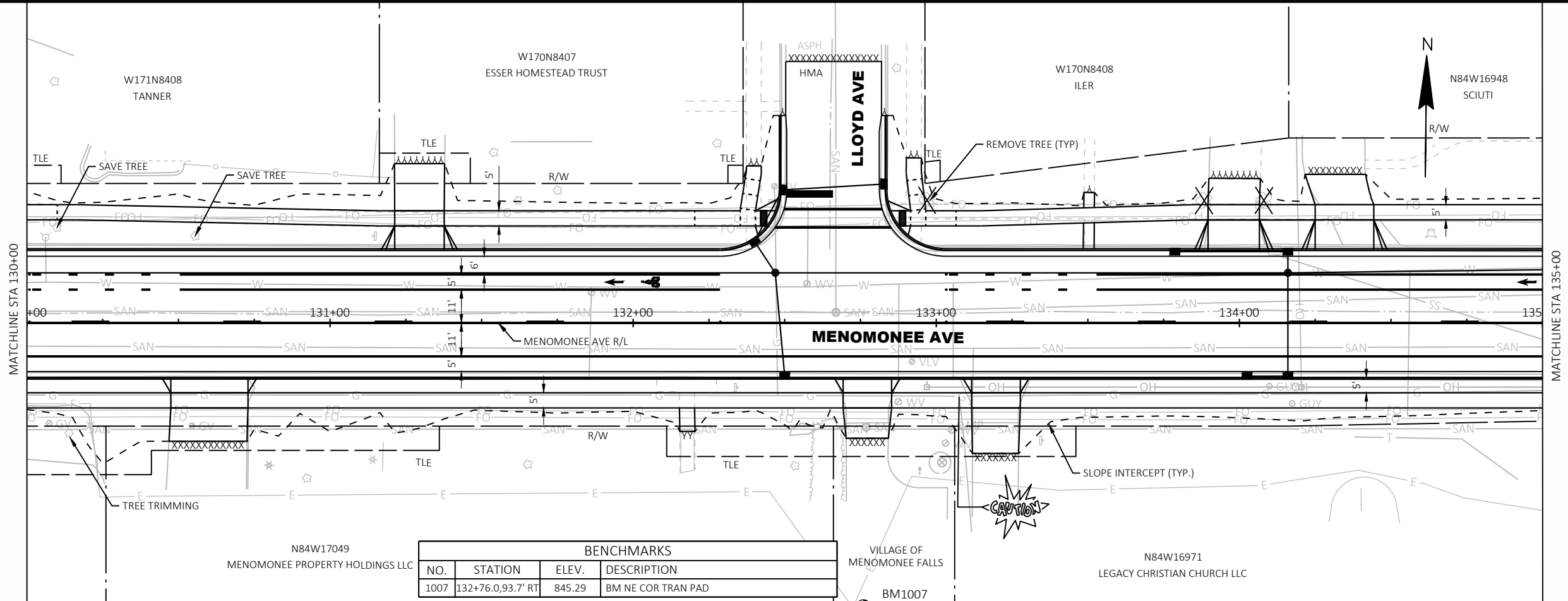


PROJECT NO: 2720-07-71	HWY: MENOMONEE AVE	COUNTY: WAUKESHA	PLAN AND PROFILE: MENOMONEE AVENUE	SHEET	E
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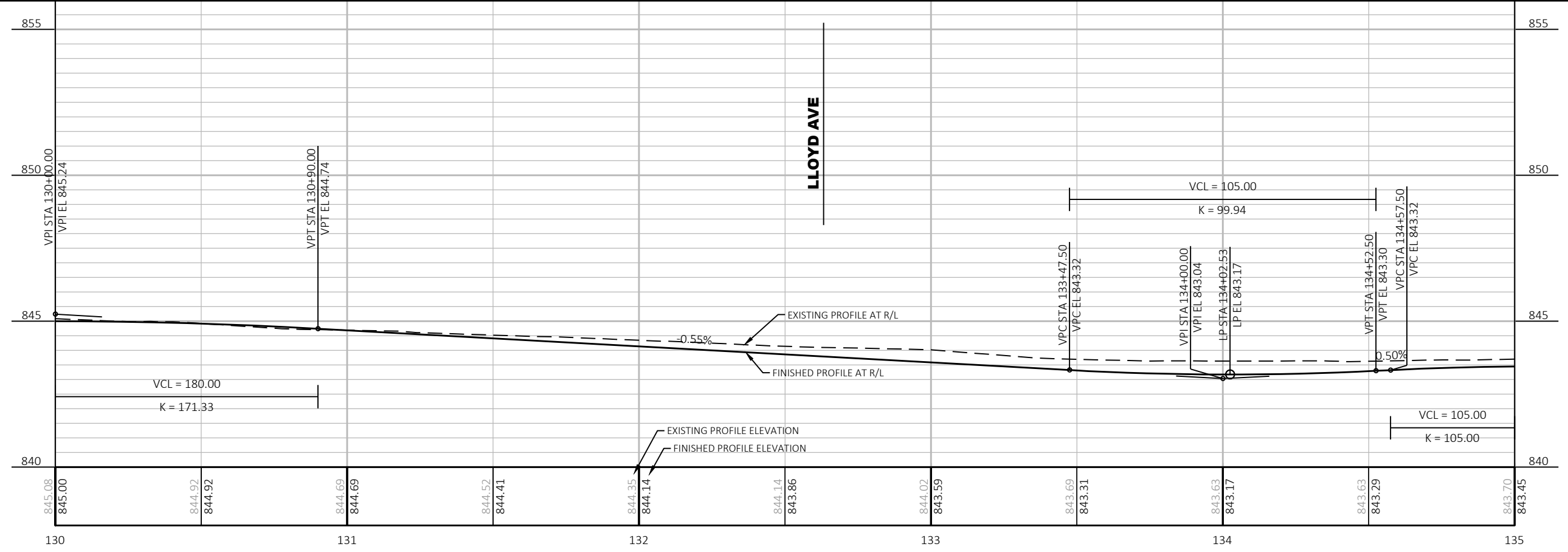


BENCHMARKS			
NO.	STATION	ELEV.	DESCRIPTION
1006	127+47.8, 37.9' RT	845.03	BM NW COR TRAN PAD

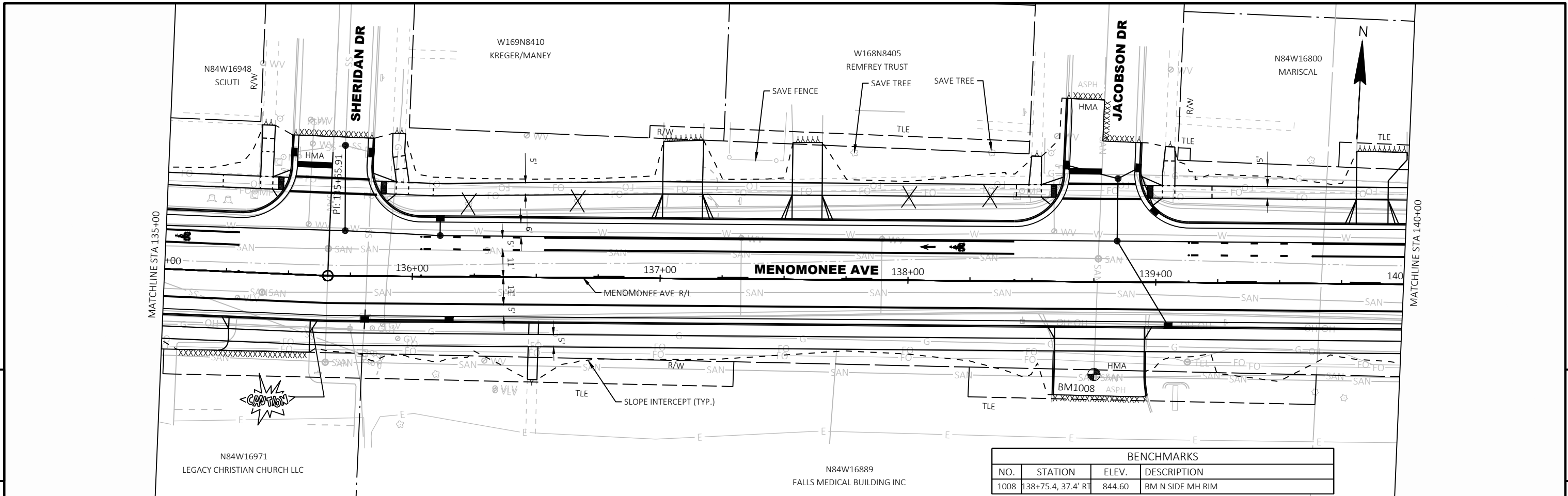




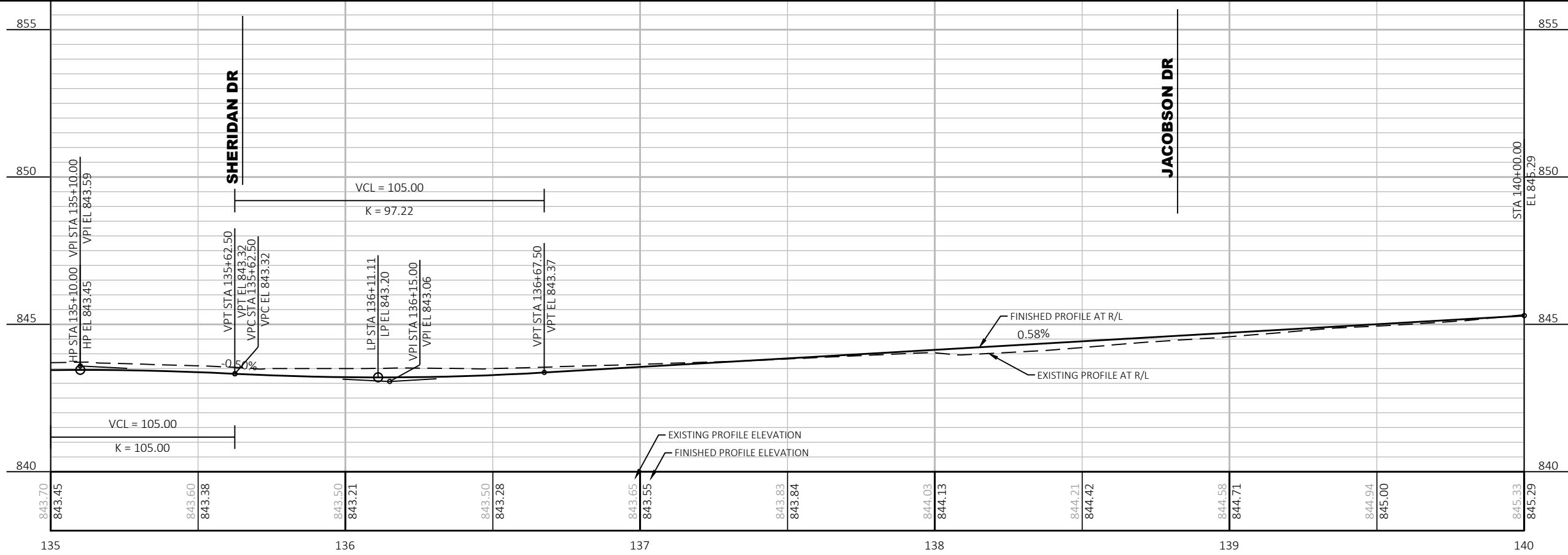
BENCHMARKS			
NO.	STATION	ELEV.	DESCRIPTION
1007	132+76.0, 93.7' RT	845.29	BM NE COR TRAN PAD



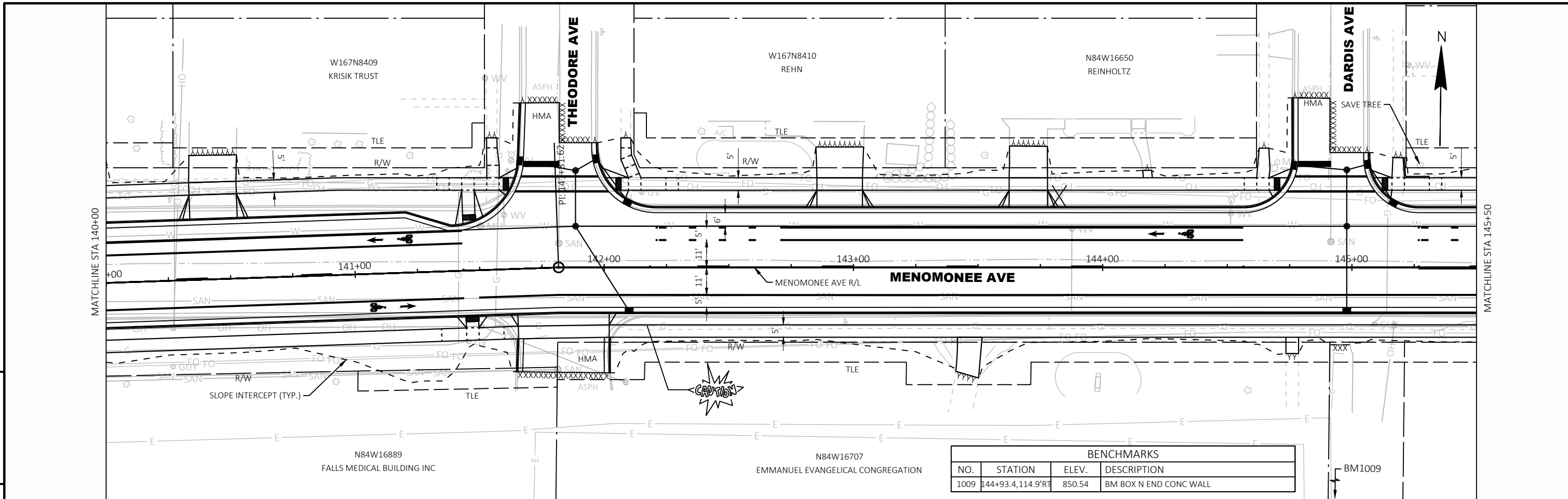
PROJECT NO: 2720-07-11 HWY: MENOMONEE AVE COUNTY: WAUKESHA PLAN AND PROFILE: MENOMONEE AVENUE SHEET: 5



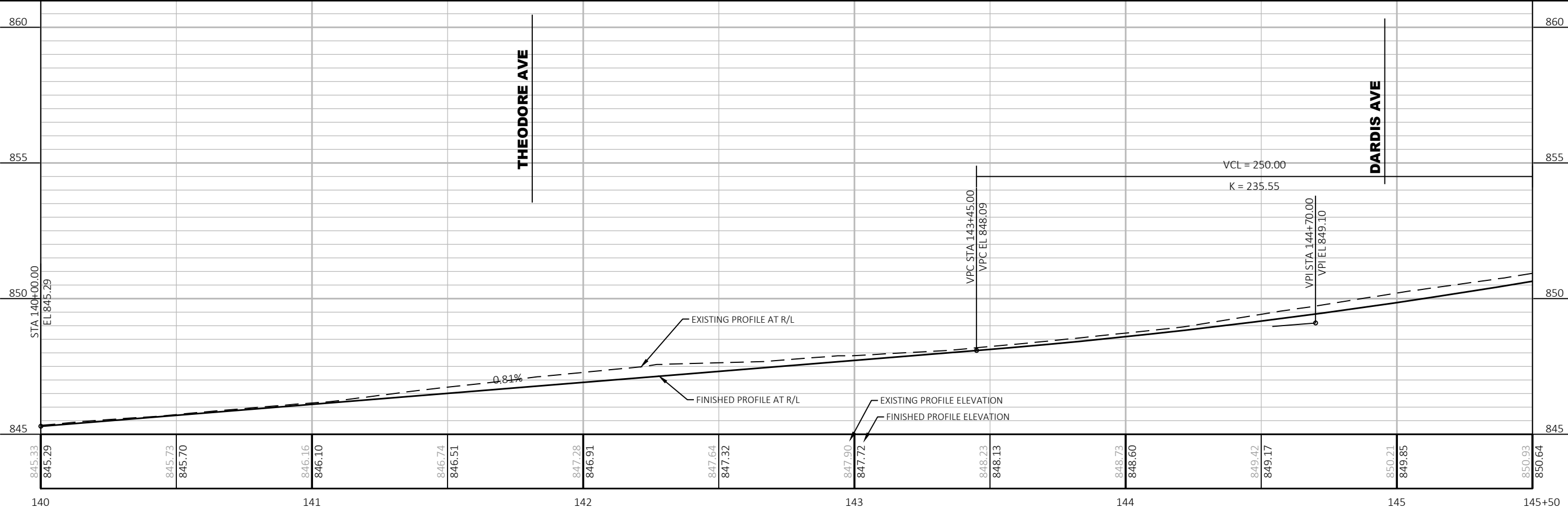
BENCHMARKS			
NO.	STATION	ELEV.	DESCRIPTION
1008	138+75.4, 37.4' RT	844.60	BM N SIDE MH RIM



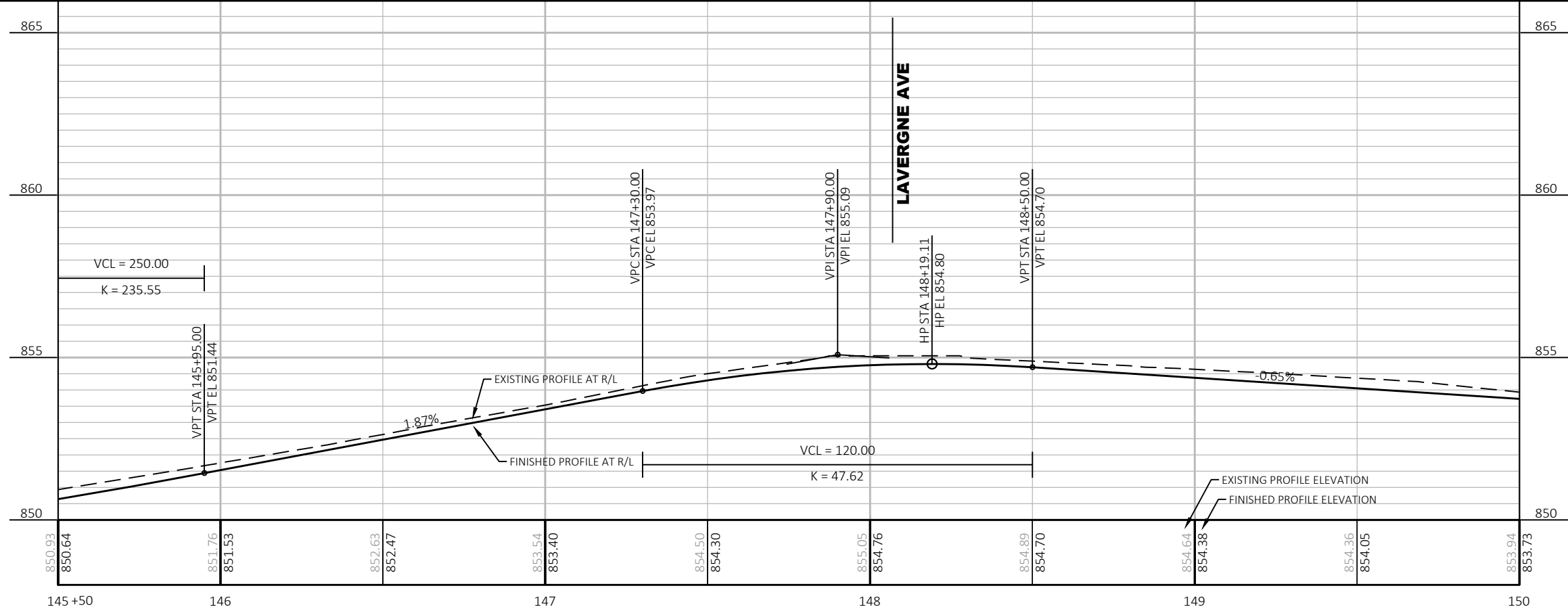
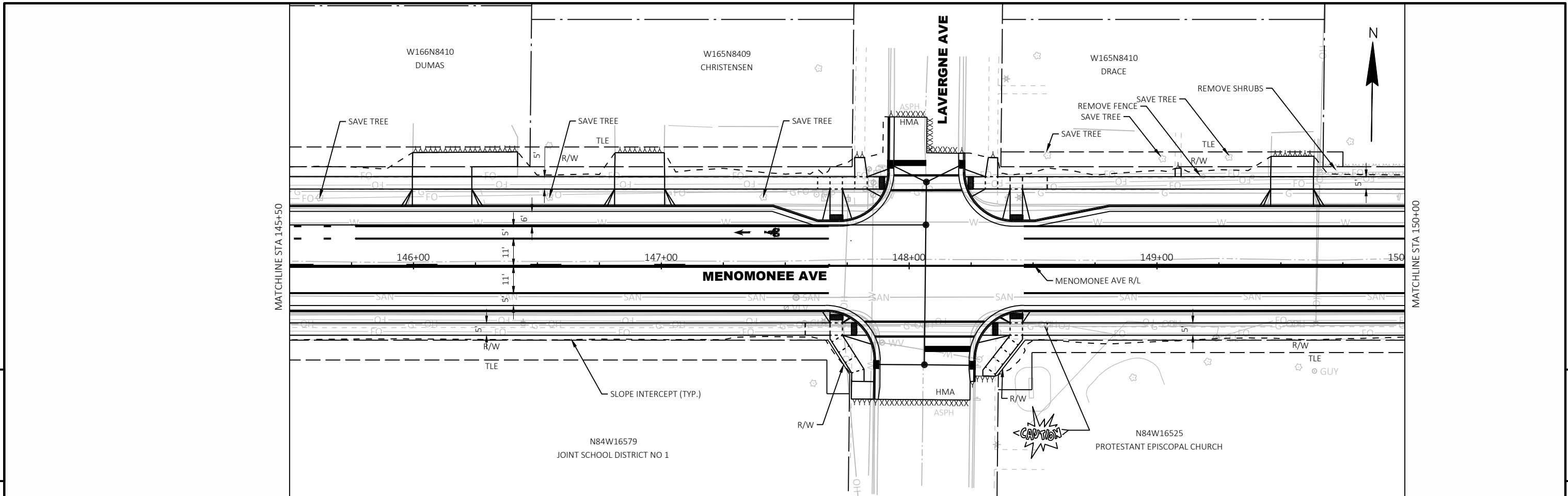
PROJECT NO: 2720-07-11 HWY: MENOMONEE AVE COUNTY: WAUKESHA PLAN AND PROFILE: MENOMONEE AVENUE SHEET: 5



BENCHMARKS			
NO.	STATION	ELEV.	DESCRIPTION
1009	144+93.4, 114.9' RT	850.54	BM BOX N END CONC WALL

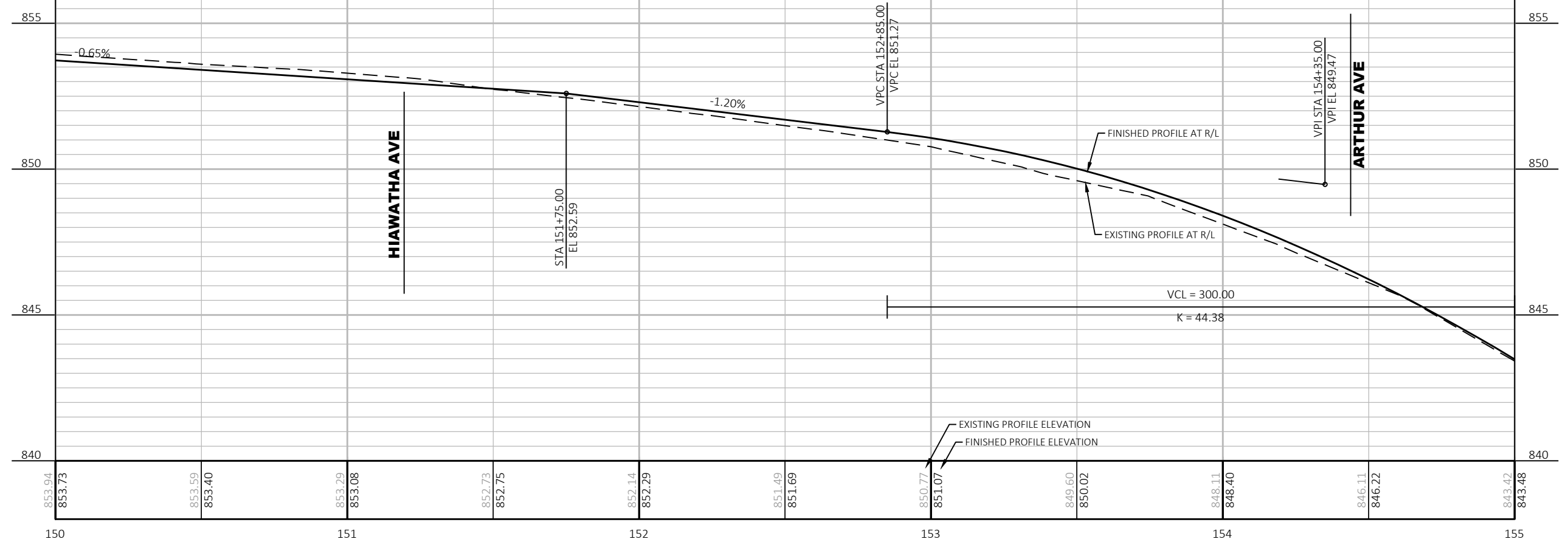
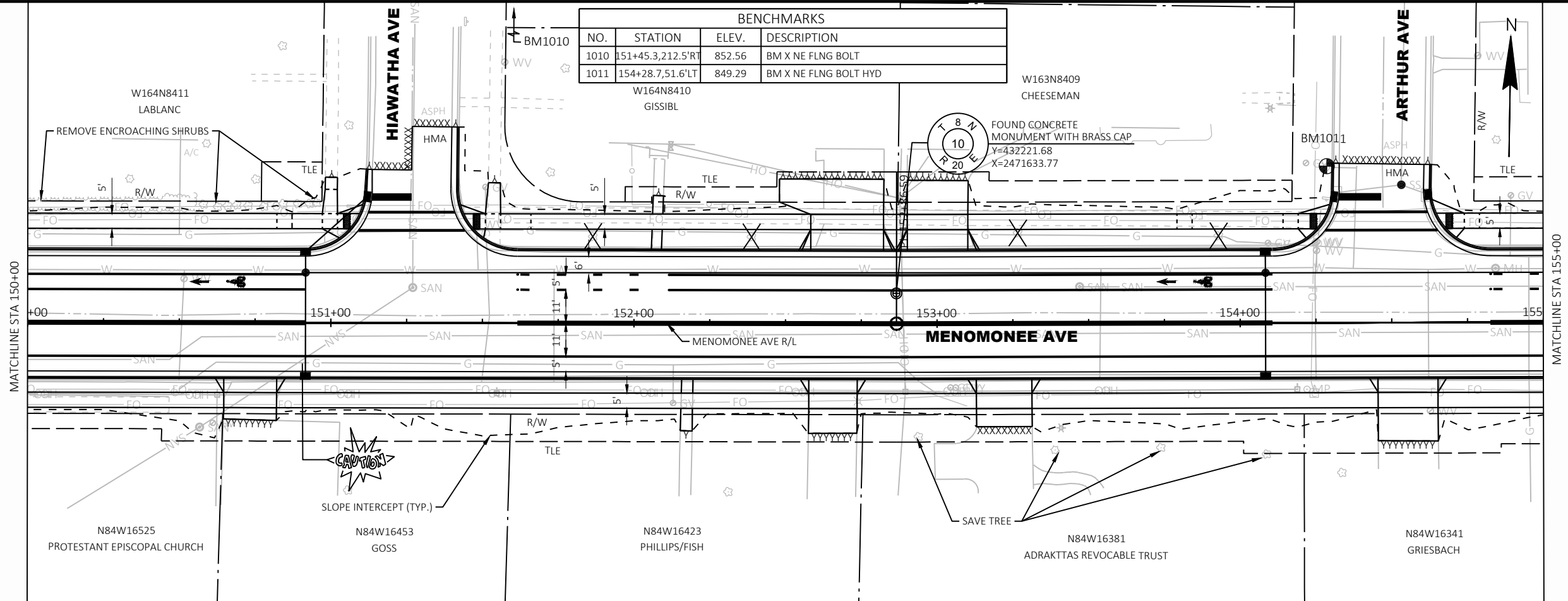


PROJECT NO: 2720-07-71	HWY: MENOMONEE AVE	COUNTY: WAUKESHA	PLAN AND PROFILE: MENOMONEE AVENUE	SHEET	E
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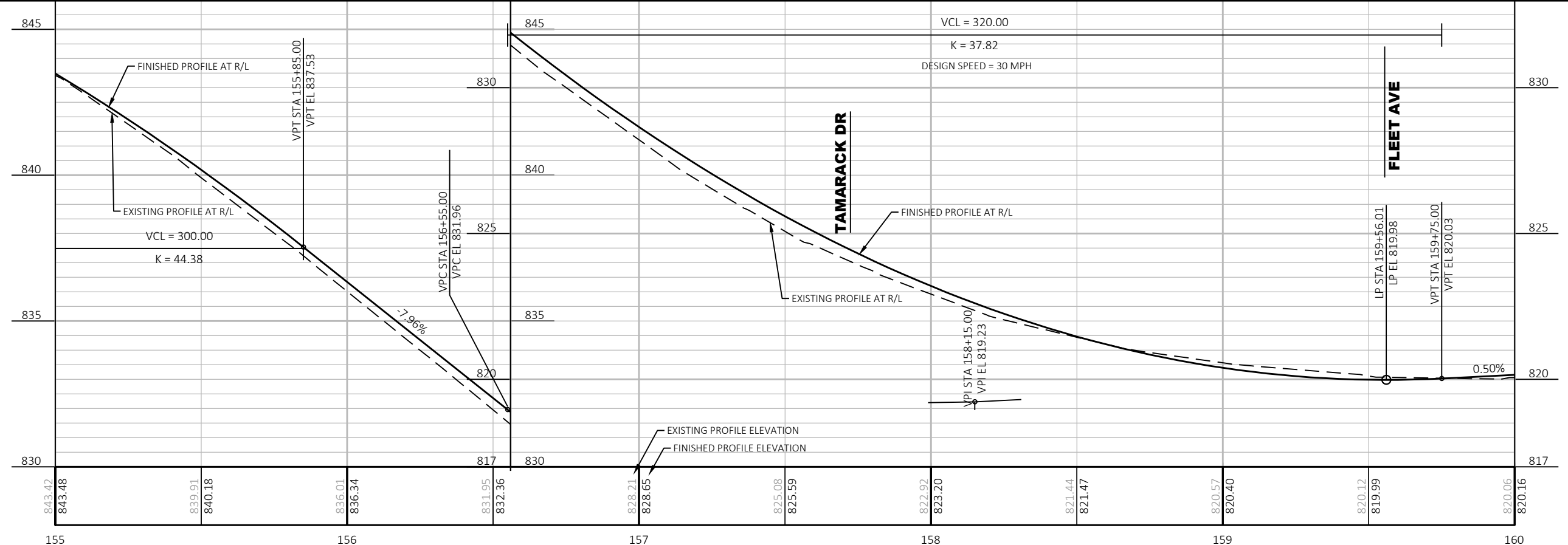
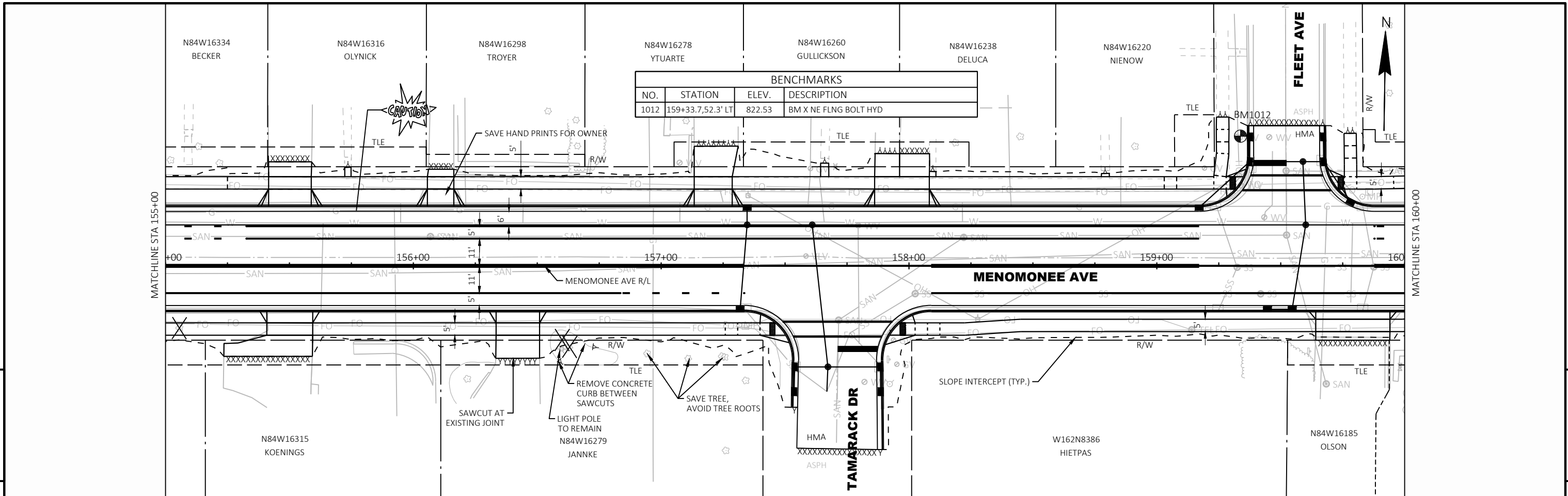


PROJECT NO: 2720-07-71	HWY: MENOMONEE AVE	COUNTY: WAUKESHA	PLAN AND PROFILE: MENOMONEE AVENUE	SHEET E
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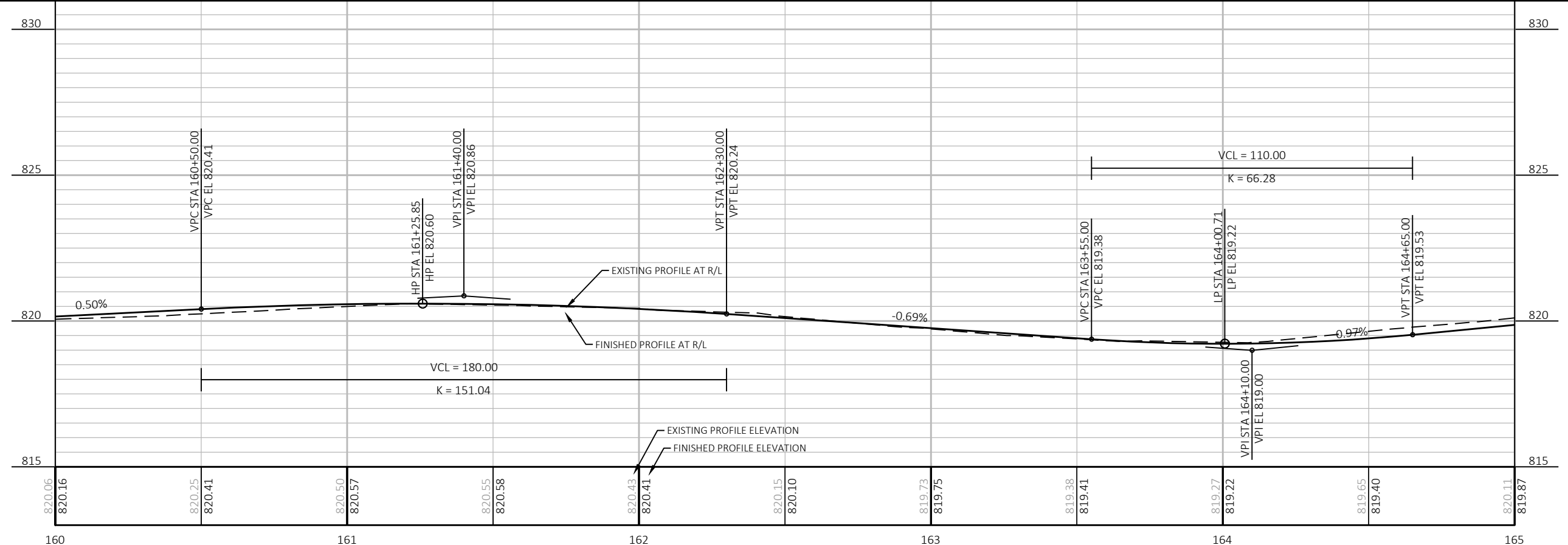
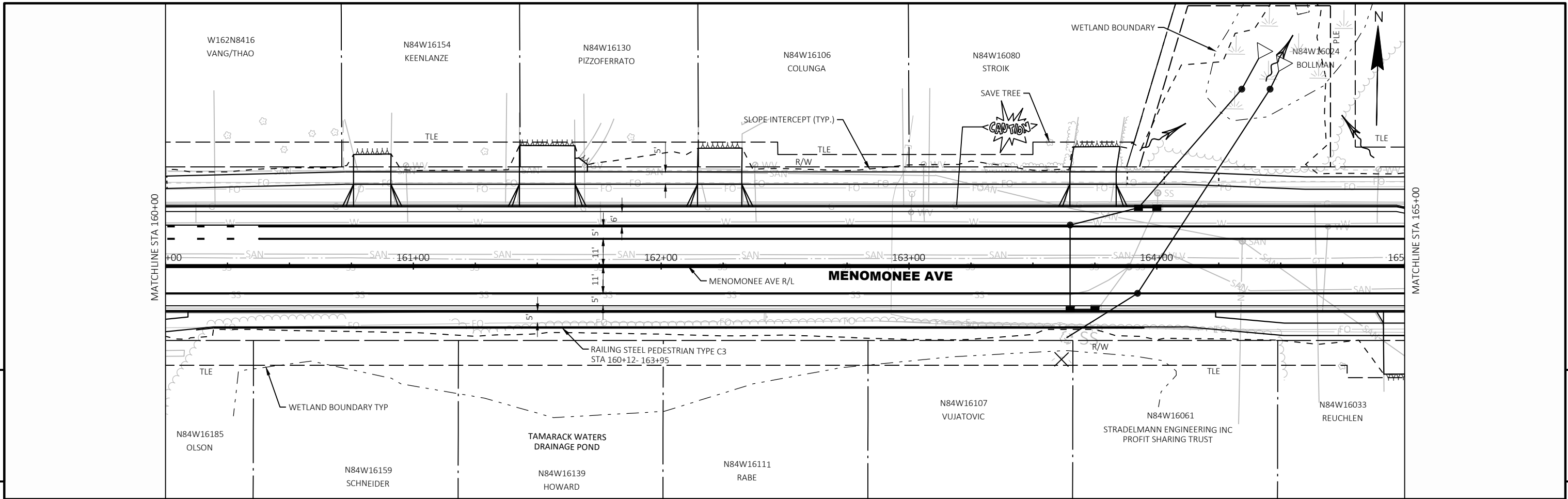
BENCHMARKS			
NO.	STATION	ELEV.	DESCRIPTION
1010	151+45.3, 212.5'RT	852.56	BM X NE FLNG BOLT
1011	154+28.7, 51.6'LT	849.29	BM X NE FLNG BOLT HYD



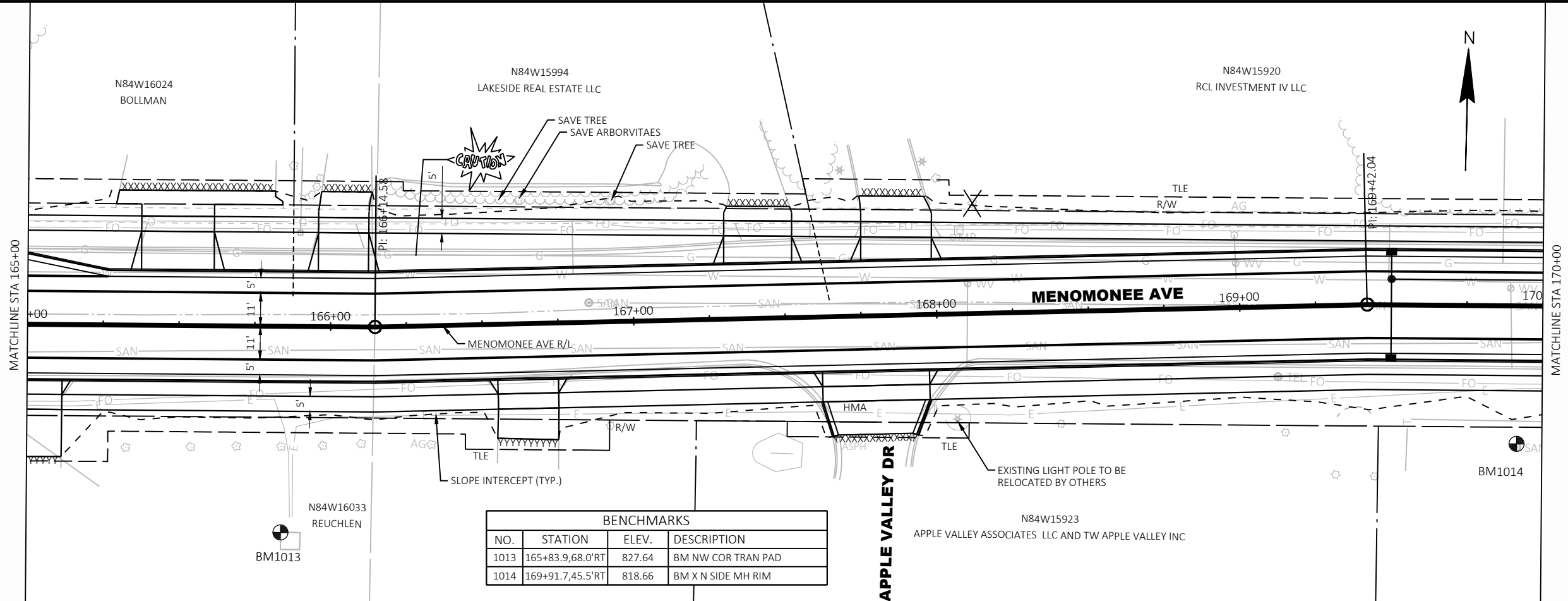
PROJECT NO: 2720-07-11 HWY: MENOMONEE AVE COUNTY: WAUKESHA PLAN AND PROFILE: MENOMONEE AVENUE SHEET: 5



PROJECT NO: 2720-07-11 HWY: MENOMONEE AVE COUNTY: WAUKESHA PLAN AND PROFILE: MENOMONEE AVENUE SHEET: 5

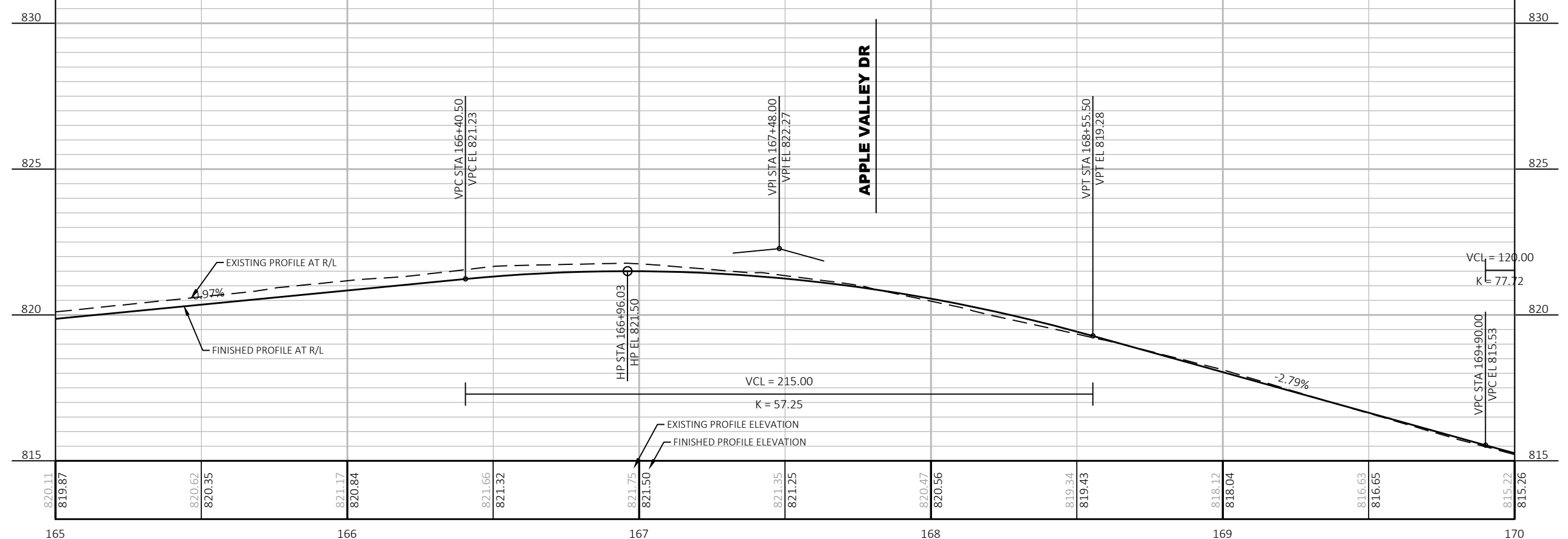


PROJECT NO: 2720-07-11 | HWY: MENOMONEE AVE | COUNTY: WAUKESHA | PLAN AND PROFILE: MENOMONEE AVENUE | SHEET: E

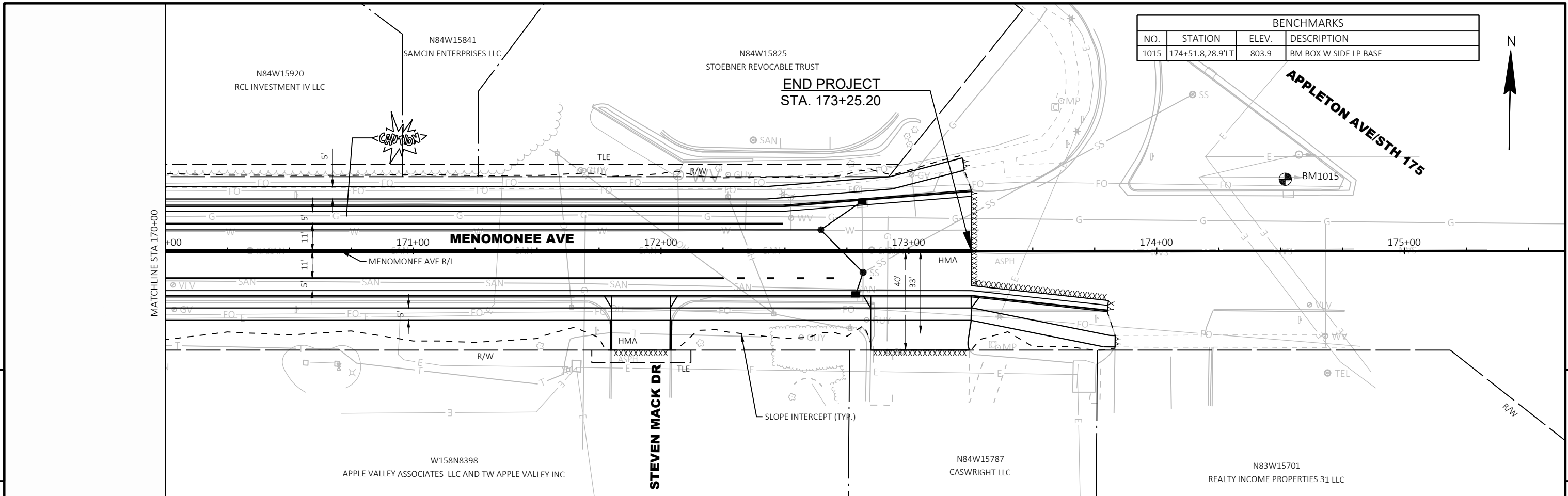


N84W16033
REUCHLEN
BM1013

BENCHMARKS			
NO.	STATION	ELEV.	DESCRIPTION
1013	165+83.9,68.0'RT	827.64	BM NW COR TRAN PAD
1014	169+91.7,45.5'RT	818.66	BM X N SIDE MH RIM



PROJECT NO: 2720-07-14 HWY: MENOMONEE AVE COUNTY: WAUKESHA PLAN AND PROFILE: MENOMONEE AVENUE SHEET: 5

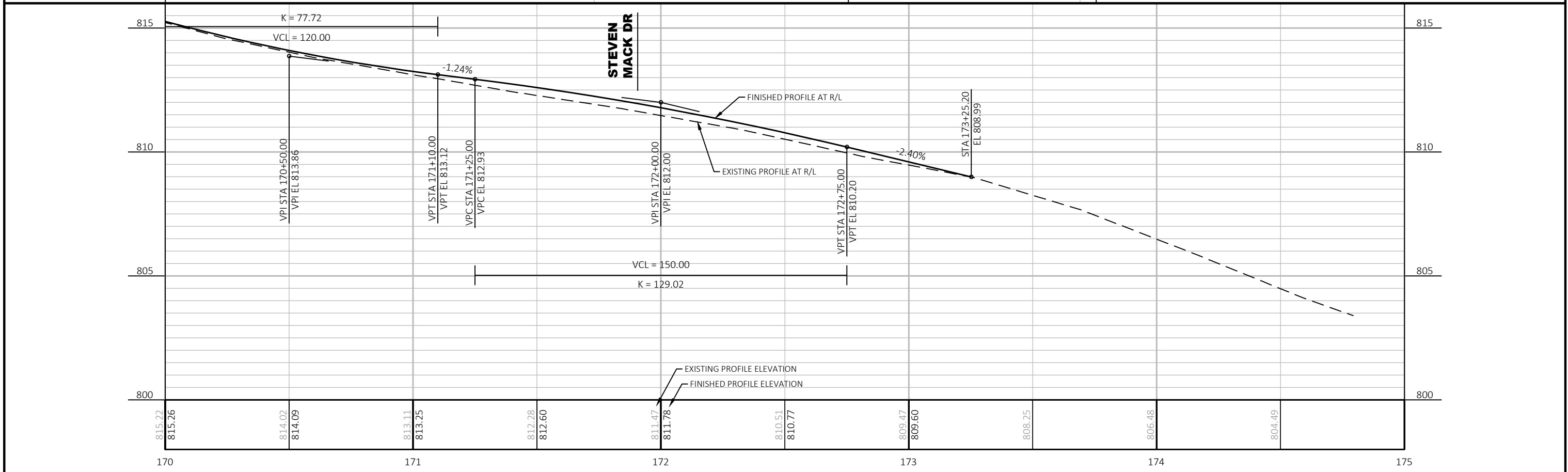


BENCHMARKS			
NO.	STATION	ELEV.	DESCRIPTION
1015	174+51.8,28.9'LT	803.9	BM BOX W SIDE LP BASE



5

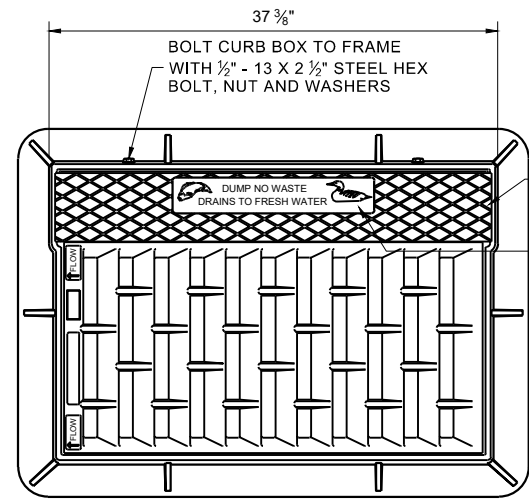
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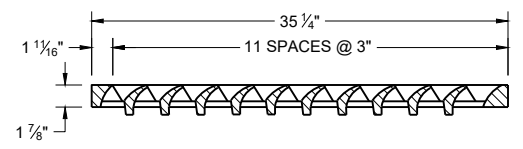
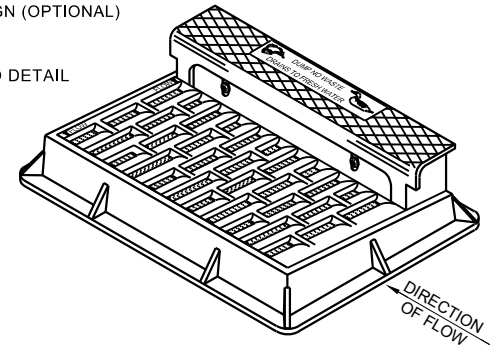
PROJECT NO: 2720-07-71	HWY: MENOMONEE AVE	COUNTY: WAUKESHA	PLAN AND PROFILE: MENOMONEE AVENUE	SHEET E
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Standard Detail Drawing List

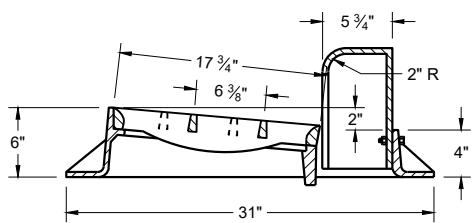
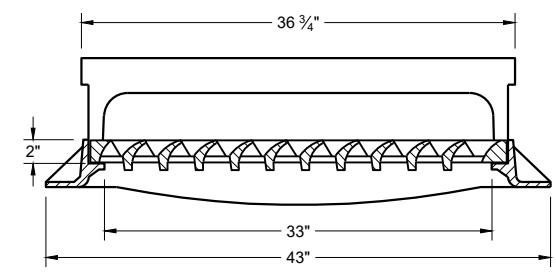
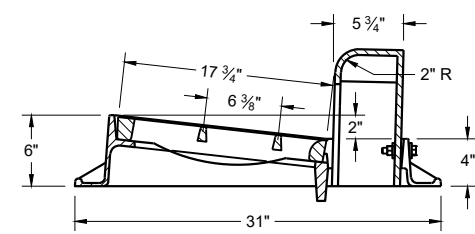
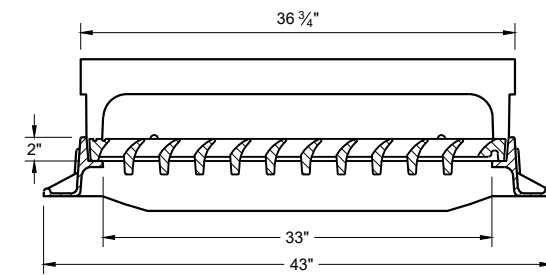
08A05-21A	INLET COVERS TYPE A, H, A-S, H-S & Z
08A05-21B	INLET COVERS TYPE B, B-A, C, MS, MS-A, & WM
08A05-21E	INLET COVER TYPE BW, MANHOLE COVERS, TYPE K, J, J-S, L & M
08A08-03	CATCH BASINS 3-FT, 4-FT, 5-FT AND 6-FT DIAMETER
08A09-03	CATCH BASINS 2X3-FT AND 2.5X3-FT
08B09-04	MANHOLES 3-FT, 4-FT, 5-FT, 6-FT, 7-FT, 8-FT, 9-FT, 10-FT DIAMETER
08D01-23A	CONCRETE CURB & GUTTER
08D01-23B	CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS
08D05-21A	CURB RAMPS TYPES 1 AND 1-A
08D05-21B	CURB RAMPS TYPES 2 AND 3
08D05-21C	CURB RAMPS TYPES 4A AND 4A1
08D05-21D	CURB RAMPS TYPE 4B AND 4B1
08D05-21E	CURB RAMPS TYPES 5, 6, 7A, 7B & 8
08D05-21F	CURB RAMPS RADIAL DETECTABLE WARNING FIELD APPLICATIONS
08D05-21G	CURB RAMPS RECTANGULAR AND RADIAL DETECTABLE WARNING PLATES
08D18-04	DRIVEWAY AND SIDEWALK RAMPS TYPES X & Y
08D19-04	DRIVEWAY AND SIDEWALK RAMPS TYPE Z
08E05-02	SODDED BACKSLOPE FLUME AND INTERCEPTING EMBANKMENT
08E08-03	TYPICAL INSTALLATIONS OF EROSION BALES / TEMPORARY DITCH CHECKS
08E09-06	SILT FENCE
08E10-02	INLET PROTECTION TYPE A, B, C AND D
08E14-01	TRACKING PAD
08F01-11	APRON ENDWALLS FOR CULVERT PIPE
08F02-01	APRON ENDWALLS FOR PIPE ARCH AND ELLIPTICAL PIPE
08F04-08	JOINT TIES FOR CONCRETE PIPE AND CONCRETE COLLAR DETAIL
11B02-02	CONCRETE MEDIAN NOSE
12A03-10	NAME PLATE (STRUCTURES)
13C19-03	HMA LONGITUDINAL JOINTS
14B07-16A	CONCRETE BARRIER TEMPORARY PRECAST, 12'-6"
14B07-16B	CONCRETE BARRIER TEMPORARY PRECAST, 12'-6"
14B07-16C	CONCRETE BARRIER TEMPORARY PRECAST, 12'-6"
14B07-16D	CONCRETE BARRIER TEMPORARY PRECAST, 12'-6"
14B07-16M	CONCRETE BARRIER TEMPORARY PRECAST, 12'-6"
14B07-16N	CONCRETE BARRIER TEMPORARY PRECAST, 12'-6"
15A04-08A	FLEXIBLE DELINEATOR POST
15C02-09A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-09B	BARRICADES AND SIGNS FOR VARIOUS CLOSURES
15C02-09C	DETOUR SIGNING FOR MAINLINE CLOSURES
15C05-05	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M.P.H. OR LESS
15C07-15C	PAVEMENT MARKING ARROWS
15C07-15E	PAVEMENT MARKING FOR BIKE LANES
15C08-23A	PERMANENT LONGITUDINAL PAVEMENT MARKINGS
15C08-23B	TEMPORARY LONGITUDINAL PAVEMENT MARKING
15C08-23C	PAVEMENT MARKING (TURN LANES)
15C08-23D	PAVEMENT MARKING (TURN LANES)
15C11-10B	CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS
15C18-08A	MEDIAN ISLAND MARKING PAVEMENT MARKINGS
15C29-08A	BICYCLE LANE MARKING
15C33-05	STOP LINE AND CROSSWALK PAVEMENT MARKING
15D30-09A	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09B	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09C	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09D	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09E	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09F	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09H	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09I	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09J	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09K	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09L	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D40-05A	TRAFFIC CONTROL, FULL LANE SHIFT NON-FREEWAY OR MULTILANE DIVIDED 45 MPH AND UNDER
15D46-01	TRAFFIC CONTROL, ONE - WAY SIGNING
16A01-07	LANDMARK REFERENCE MONUMENTS AND COVERS



NOTE: EITHER CASTING IS ACCEPTABLE

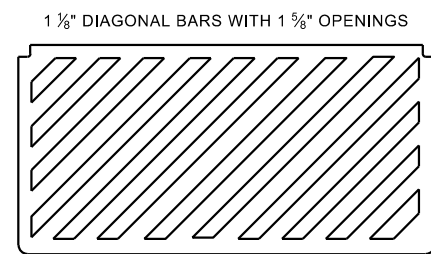


NOTE: CURB BOX HEIGHT ADJUSTABLE 6" - 9"



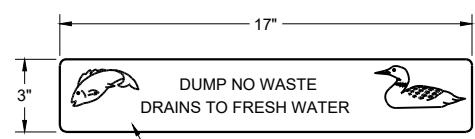
TYPE "H"

NOTE: EITHER CASTING IS ACCEPTABLE



SPECIAL GRATE FOR TYPE "H" COVER

(MEASURES 35" X 17 3/4" X 2")
(NOTED AS TYPE H-S ON DRAINAGE TABLE)



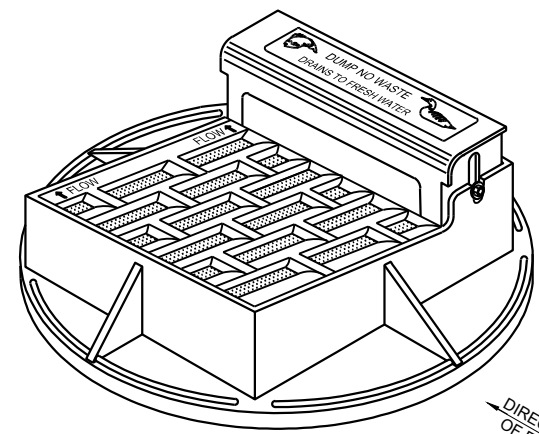
LOGO DETAIL

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

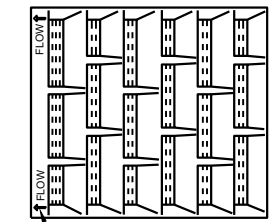
DETAIL DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR CATCH BASIN, MANHOLE AND INLET COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ROUND FRAMES AND COVERS SHALL HAVE CONTINUOUSLY MACHINED BEARING SURFACES TO PREVENT ROCKING AND RATTLING.

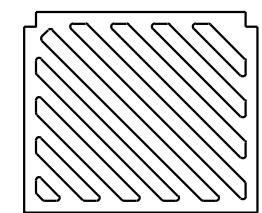


NOTE: CURB BOX HEIGHT ADJUSTABLE 6" - 9"

NOTE: EITHER CASTING IS ACCEPTABLE

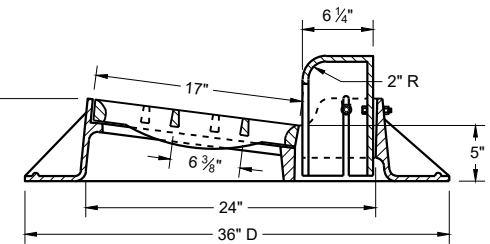
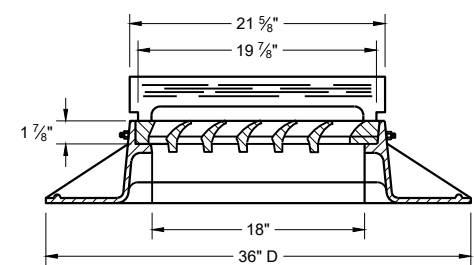
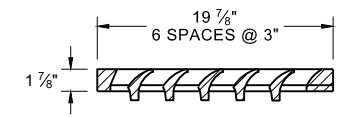


1" DIAGONAL BARS WITH 1 1/2" OPENINGS

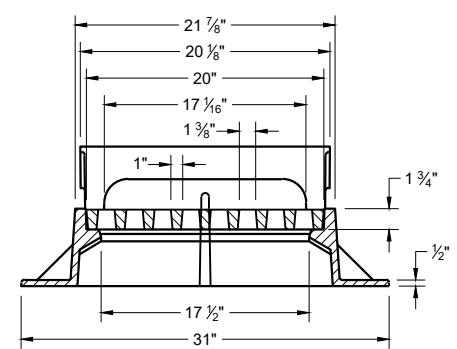
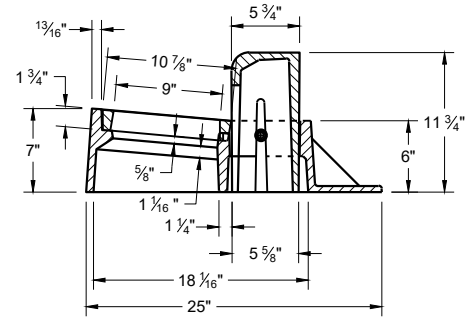


SPECIAL GRATE FOR TYPE "A" COVER

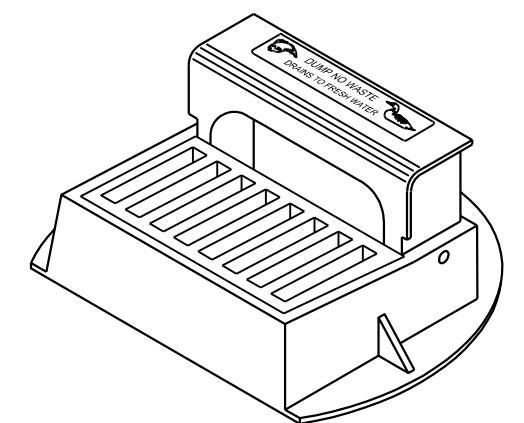
(MEASURES 19 3/4" X 17" X 1 7/8")
(NOTED AS TYPE A-S ON DRAINAGE TABLE)



TYPE "A"



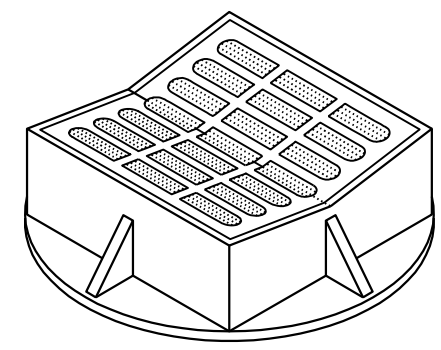
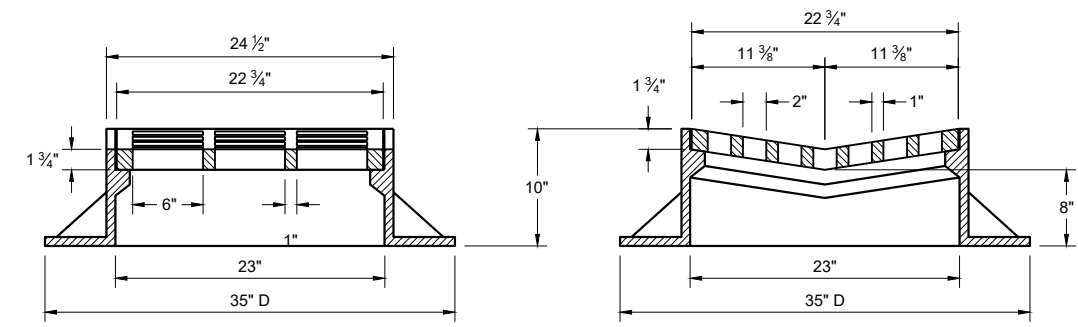
TYPE "Z"



INLET COVERS TYPES A, H, A-S, H-S AND Z

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

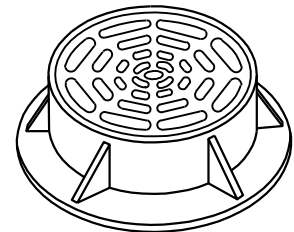
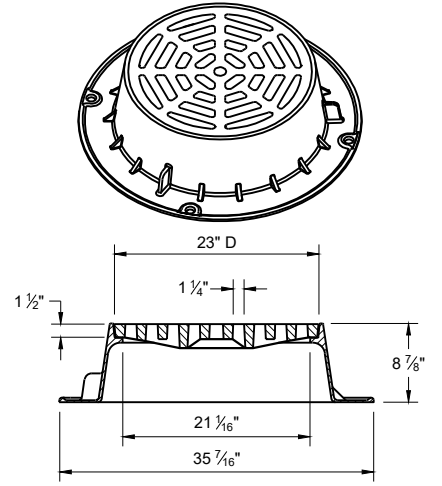
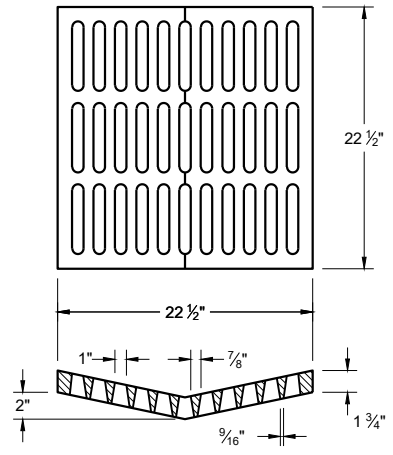
APPROVED
December 2023 DATE /S/ Rodney Taylor
ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR
FHWA



TYPE "B"

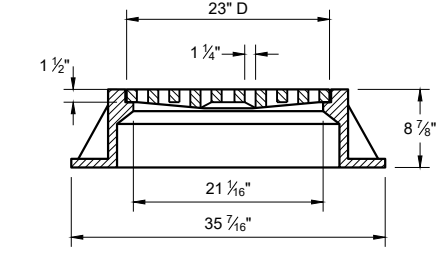
ALTERNATIVE GRATE FOR TYPE "B" COVER

USE WHERE PEDESTRIAN OR BICYCLE TRAFFIC IS POSSIBLE
NOTED AS TYPE B - A ON THE DRAINAGE TABLE



TYPE "C"

NOTE: EITHER CASTING IS ACCEPTABLE

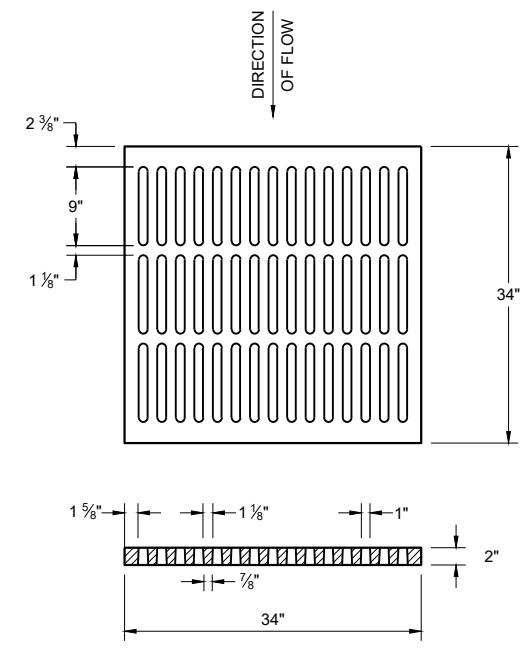


GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

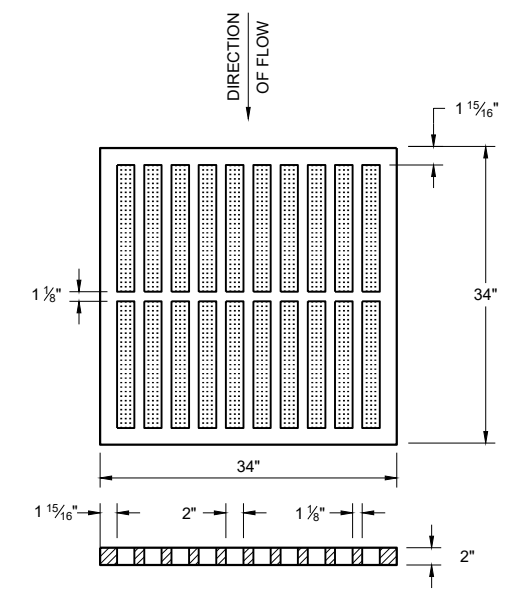
DETAIL DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR CATCH BASIN, MANHOLE AND INLET COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ROUND FRAMES AND COVERS SHALL HAVE CONTINUOUSLY MACHINED BEARING SURFACES TO PREVENT ROCKING AND RATTLING.



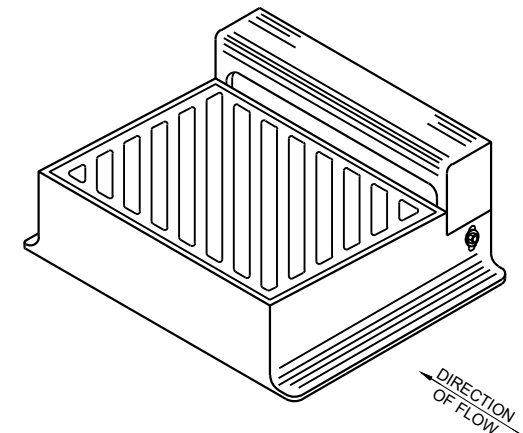
ALTERNATIVE TYPE "MS"

USE WHERE PEDESTRIAN OR BICYCLE TRAFFIC IS PERMITTED
NOTED AS TYPE MS-A ON THE DRAINAGE TABLE

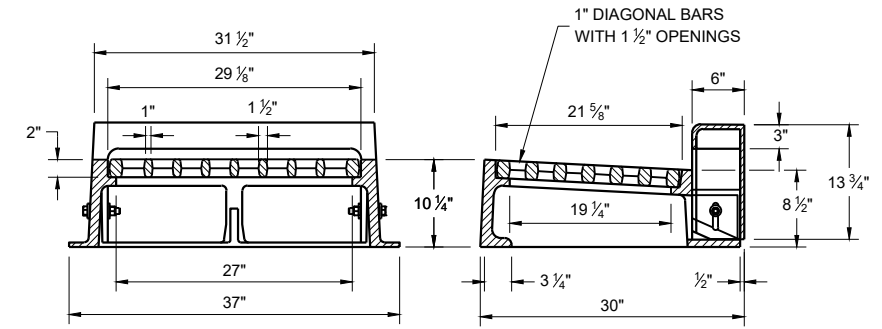


TYPE "MS"

USE ON FREEWAYS AND EXPRESSWAYS
NOTED AS TYPE MS ON THE DRAINAGE TABLE



DIAGONAL SLOTS SHALL BE ORIENTED TO THE DIRECTION OF FLOW AS ILLUSTRATED.
 GRATES ARE MANUFACTURED TO BE REVERSIBLE.



TYPE "WM"

NOTE: CURB BOX HEIGHT ADJUSTABLE 6" - 9"

6

6

SDD 08A05-21b

SDD 08A05-21b

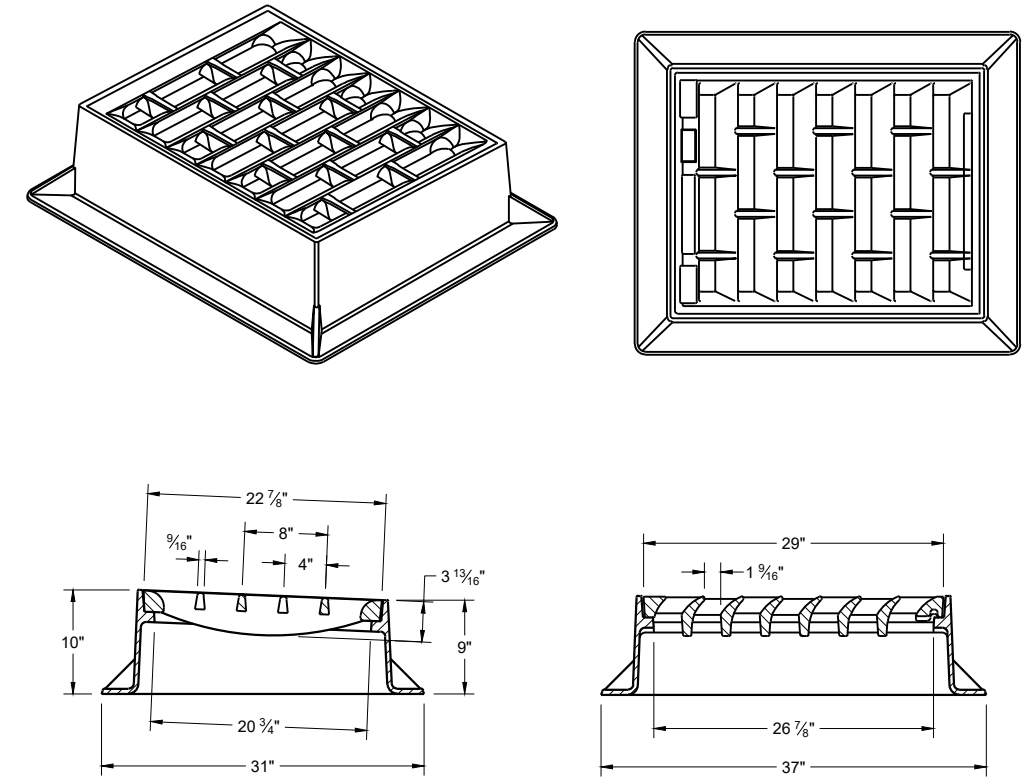
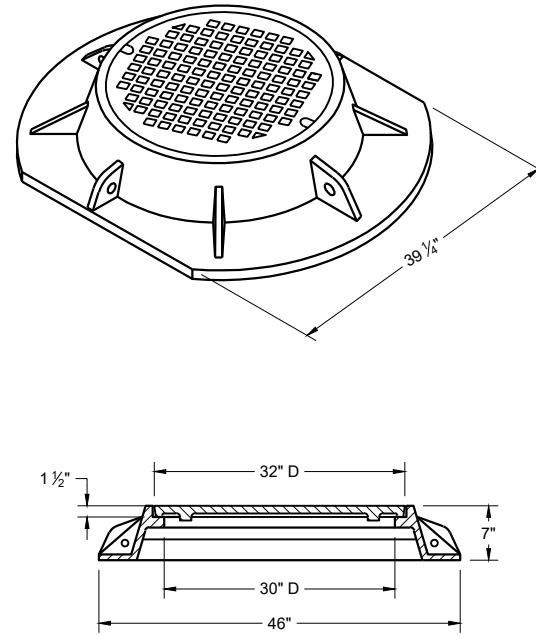
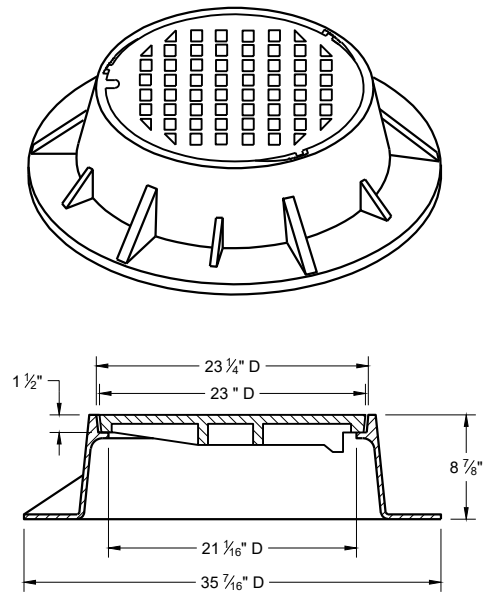
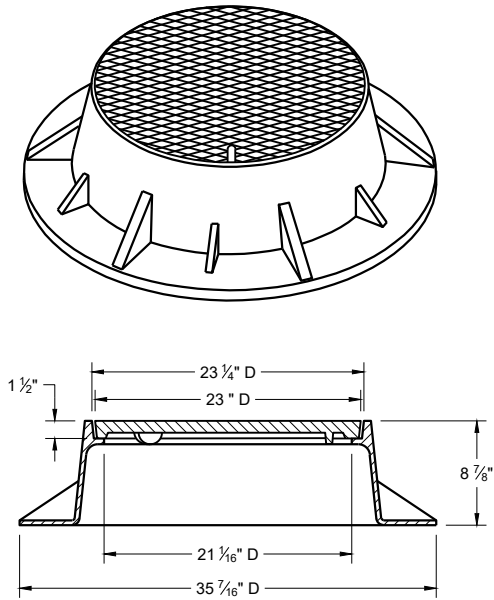
INLET COVERS TYPES B, B-A, C, MS, MS-A AND WM	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED December 2023 DATE	/s/ Rodney Taylor ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR
FHWA	

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

DETAIL DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR MANHOLE COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ROUND FRAMES AND COVERS SHALL HAVE CONTINUOUSLY MACHINED BEARING SURFACES TO PREVENT ROCKING AND RATTLING.

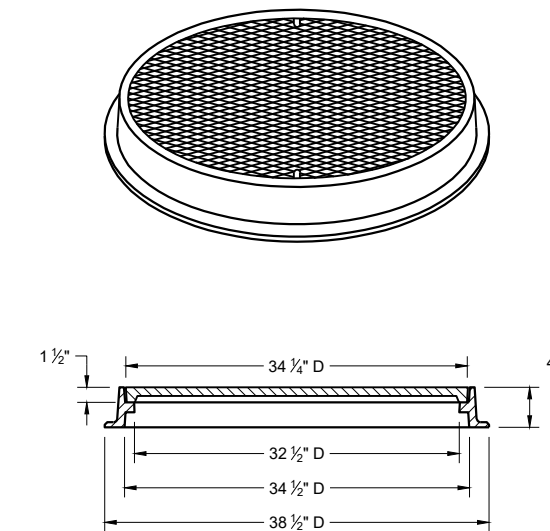
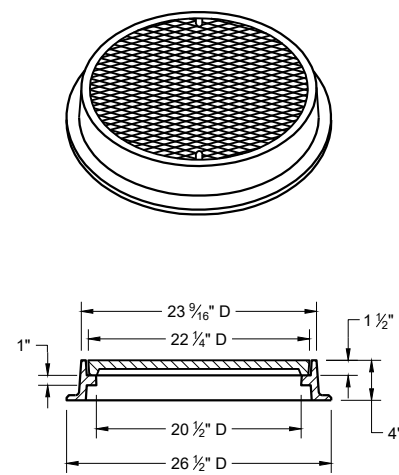
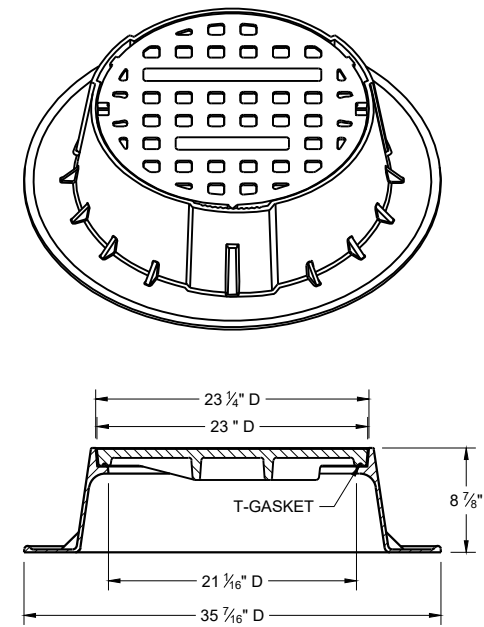
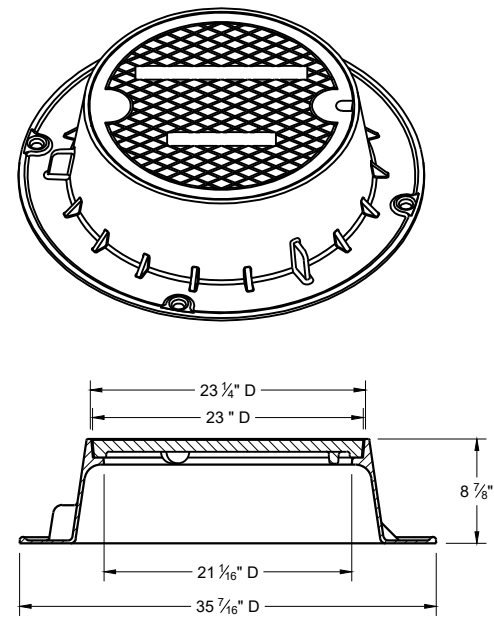


TYPE "K"

INLET COVER TYPE "BW"

6

6



TYPE "J"

NOTE: EITHER CASTING IS ACCEPTABLE

TYPE "J" SPECIAL

TYPE "B" NON-ROCKING SELF-SEAL LID (NOTED AS TYPE J-S ON THE DRAINAGE TABLE)

NOTE: EITHER CASTING IS ACCEPTABLE

TYPE "L"

TYPE "M"

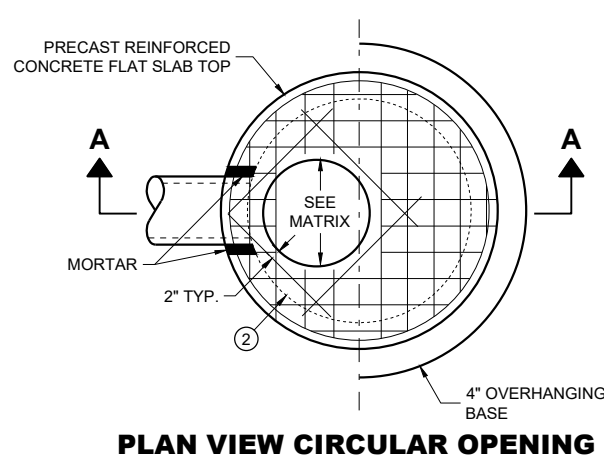
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SDD 08A05-21e

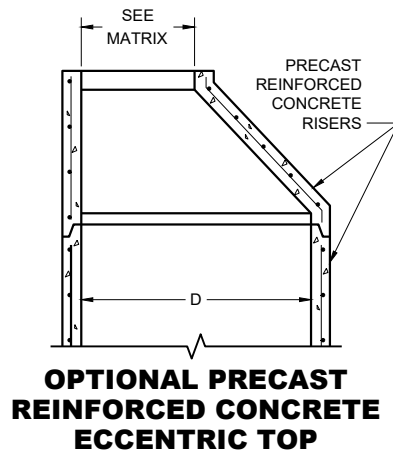
**INLET COVERS TYPES BW
MANHOLE COVERS TYPES K,
J, J-S, L, AND M**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

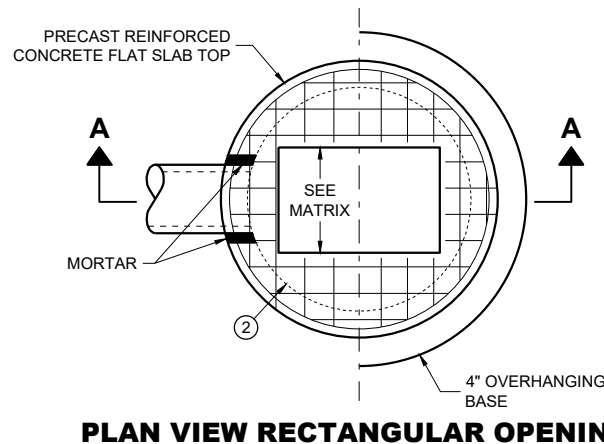
APPROVED
December 2023 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
FHWA UNIT SUPERVISOR



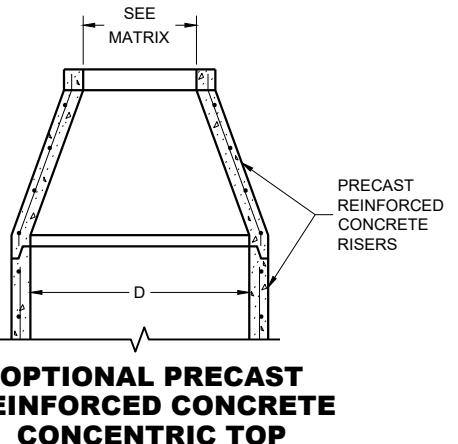
PLAN VIEW CIRCULAR OPENING



OPTIONAL PRECAST REINFORCED CONCRETE ECCENTRIC TOP



PLAN VIEW RECTANGULAR OPENING



OPTIONAL PRECAST REINFORCED CONCRETE CONCENTRIC TOP

COVER MATRIX

CATCH BASIN SIZE	INLET COVER TYPE	ALL A'S	ALL B'S	BW	C	F	ALL H'S	S	T	V V-B	VV-B	WM	Z
		OPENING SIZE (FT.)											
3-FT	2 X 2	X	X					X		X			
	2 DIA.				X								X
4-FT TO 6-FT	2 X 2	X	X					X		X			
	2 X 2.5			X				X	X	X		X	
	2 DIA.				X								X
	2 X 3						X						
	2.5 X 3					X							
	2 X 3.5*										X*		

* REQUIRES 5-FT DIAMETER OR LARGER STRUCTURE

PIPE MATRIX

CATCH BASIN SIZE	MAXIMUM INSIDE PIPE DIAMETER FOR TWO PIPES	
	180° SEPARATION (IN)	90° SEPARATION (IN)
3-FT	15	12
4-FT	24	18
5-FT	36	24
6-FT	42	30

CATCH BASINS 3-FT, 4-FT, 5-FT AND 6-FT DIAMETER

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST CATCH BASIN UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL DRAINAGE STRUCTURES ARE DESIGNATED ON THE PLANS AS "MANHOLES 3X3-L", "CATCH BASINS 4-B", "INLETS 2X3-H", ETC. THE FIRST NUMBERS DESIGNATE THE SIZE OF THE STRUCTURE, AND THE FOLLOWING LETTER DESIGNATES THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF FOUNDATION BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

PRECAST REINFORCED CONCRETE CONE TOPS (ECCENTRIC OR CONCENTRIC) OR PRECAST REINFORCED CONCRETE FLAT SLAB TOPS MAY BE USED ON CONCRETE BLOCK STRUCTURES.

ECCENTRIC CONE TOPS MAY BE USED ON ALL STRUCTURES. CONCENTRIC CONE TOPS SHALL BE USE ONLY ON STRUCTURES 5 FEET OR LESS IN DEPTH UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

STEPS MEETING AASHTO M199 AND THE FOLLOWING REQUIREMENTS SHALL BE INSTALLED IN ALL STRUCTURES OVER 5 FEET IN DEPTH: 16 INCH C-C MAXIMUM SPACING; PROJECT A MINIMUM CLEAR DISTANCE OF 4 INCHES FROM THE WALL AT THE POINT OF EMBEDMENT; MINIMUM LENGTH OF 10 INCHES; MINIMUM WALL EMBEDMENT OF 3 INCHES. FERROUS METAL STEPS NOT PAINTED OR TREATED TO RESIST CORROSION SHALL HAVE A MINIMUM CROSS SECTIONAL DIMENSION OF 1 INCH.

STEPS OF APPROVED POLYPROPYLENE PLASTIC COATED REINFORCEMENT BAR ARE ACCEPTABLE. REINFORCING BAR MUST BE A MINIMUM OF 1/2 INCH AND MEET THE REQUIREMENTS OF ASTM A615.

CERTIFICATION SHALL BE PROVIDED THAT INSTALLED STEPS WHEN TESTED IN ACCORDANCE WITH SECTION 10 OF AASHTO T280 CAN WITHSTAND A VERTICAL LOAD OF 800 LBS. AND A HORIZONTAL LOAD OF 400 LBS.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

ALL PRECAST INLET UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF AASHTO DESIGNATION M199.

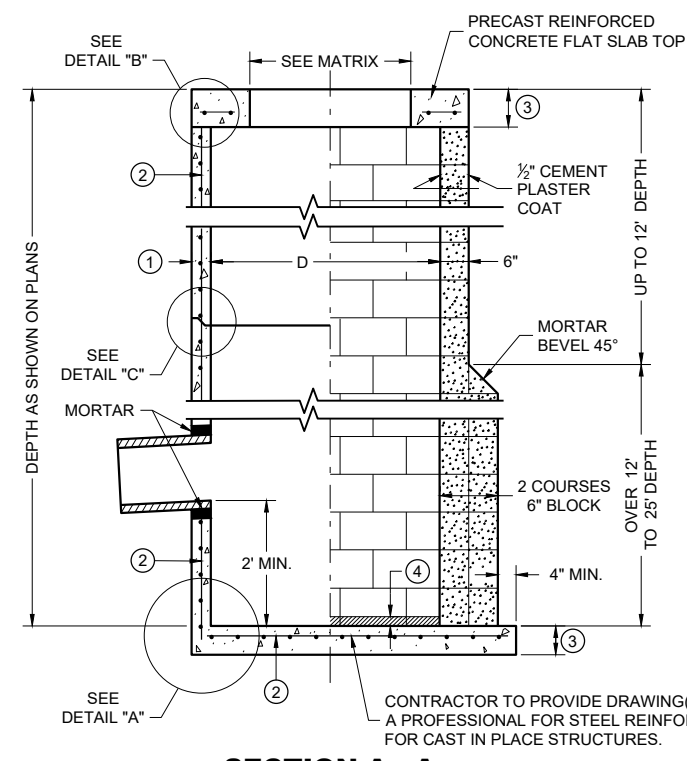
PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

CONCRETE BLOCK WILL NOT BE PERMITTED FOR STRUCTURES GREATER THAN 4 FEET IN DIAMETER.

4" OVERHANGING BASES ARE REQUIRED FOR ALL CONCRETE BLOCK INSTALLATIONS. 4" OVERHANG IS REQUIRED WHEN SEPARATE PRECAST BASE IS PROVIDED. OVERHANG IS NOT REQUIRED ON PRECAST STRUCTURES WITH AN INTEGRAL OR MONOLITHIC BASE.

FOR ADDITIONAL CONFIGURATIONS, MAINTAIN A MINIMUM OF 12 INCHES AS MEASURED FROM THE INSIDE OF THE STRUCTURE WALL BETWEEN THE OUTSIDE PIPE WALLS OF ADJACENT PIPES. SEE DETAIL "D".

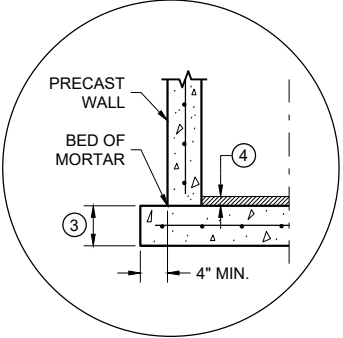
- ① MINIMUM WALL THICKNESS SHALL BE 4 INCHES FOR 3-FT, 5 INCHES FOR 4-FT, 6 INCHES FOR 5-FT AND 7 INCHES FOR 6-FT DIAMETER PRECAST CATCH BASINS.
- ② FOR PRECAST CATCH BASINS AND REINFORCED CONCRETE BASES PROVIDE REINFORCING STEEL IN ACCORDANCE TO AASHTO M199.
- ③ PRECAST FLAT SLAB TOPS AND BASES WITH A DIAMETER OF 48" AND LESS SHALL HAVE A MINIMUM THICKNESS OF 6". PRECAST FLAT SLAB TOPS AND BASES WITH A DIAMETER LARGER THAN 48" SHALL HAVE A MINIMUM THICKNESS OF 8".
- ④ 1" CONCRETE KEY POURED AFTER INSTALLATION. 2' SUMP MEASURED FROM TOP OF KEY.
- ⑤ JOINTS TO BE SEALED WITH A BUTYL RUBBER SEAL PER SEALANT MANUFACTURERS RECOMMENDATIONS CONFORMING TO ASTM C 990 OR RUBBER GASKETS CONFORMING TO ASTM C443.



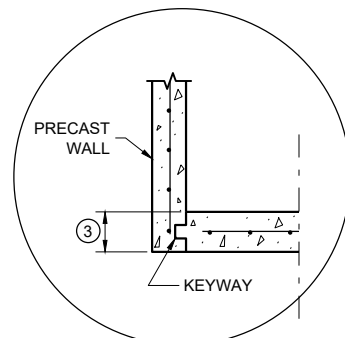
SECTION A - A

PRECAST REINFORCED CONCRETE WITH MONOLITHIC BASE

CONCRETE BLOCK WITH CAST IN PLACE OR PRECAST REINFORCED CONCRETE BASE ②

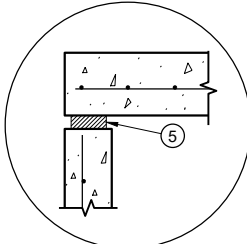


SEPARATE PRECAST REINFORCED CONCRETE BASE OPTION



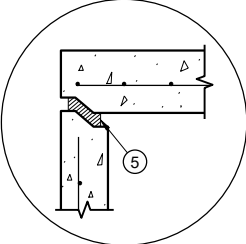
PRECAST REINFORCED CONCRETE WITH INTEGRAL BASE OPTION

DETAIL "A"



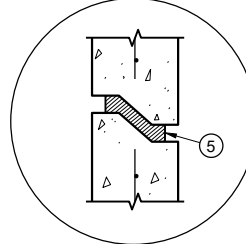
TOP WITH PLAIN END JOINT

DETAIL "B"



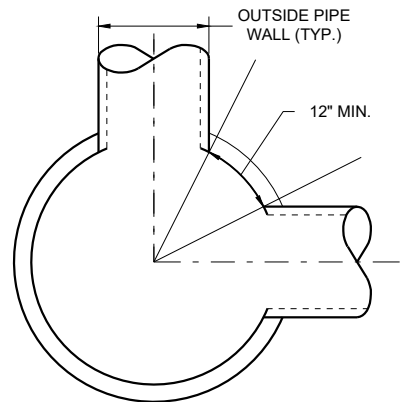
RISER WITH TONGUE AND GROOVE JOINT

DETAIL "C"



TOP WITH TONGUE AND GROOVE JOINT

RISER WITH TONGUE AND GROOVE JOINT



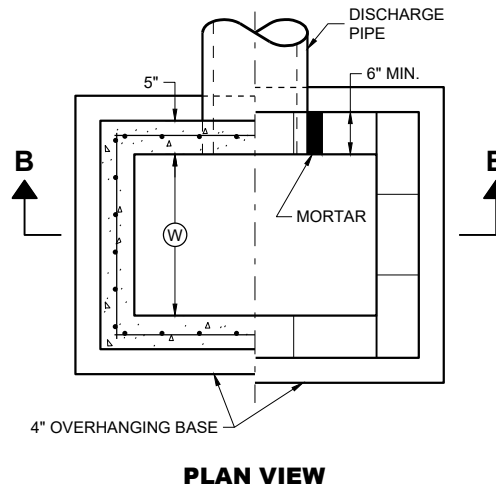
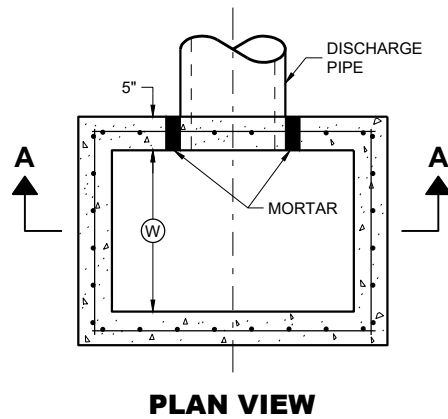
MINIMUM HORIZONTAL PIPE SEPARATION

DETAIL "D"

CATCH BASINS, 3-FT. 4-FT., 5 FT., AND 6-FT. DIAMETER

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED December 2023 DATE /S/ Rodney Taylor ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR



GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST CATCH BASIN UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL PRECAST CATCH BASIN UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF ASTM C 913.

ALL DRAINAGE STRUCTURES ARE DESIGNATED ON THE PLANS AS "MANHOLES 3X3-L", "CATCH BASINS 4-B", "INLETS 2X3-H", ETC. THE FIRST NUMBERS DESIGNATES THE SIZE OF THE STRUCTURE, AND THE FOLLOWING LETTER DESIGNATES THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF FOUNDATION BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

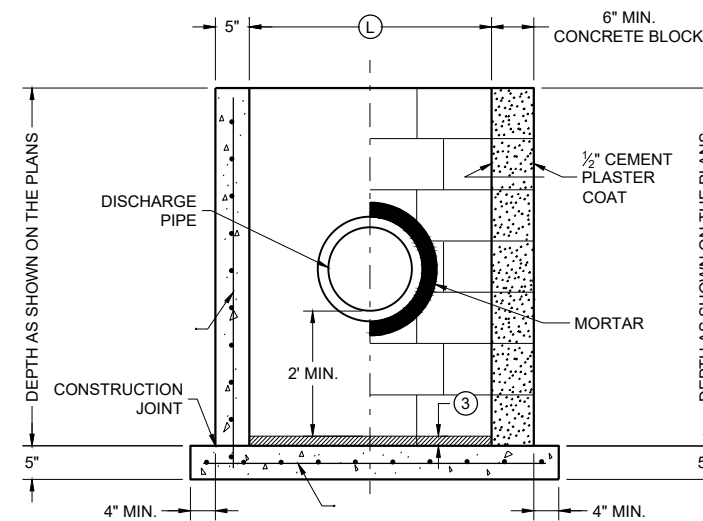
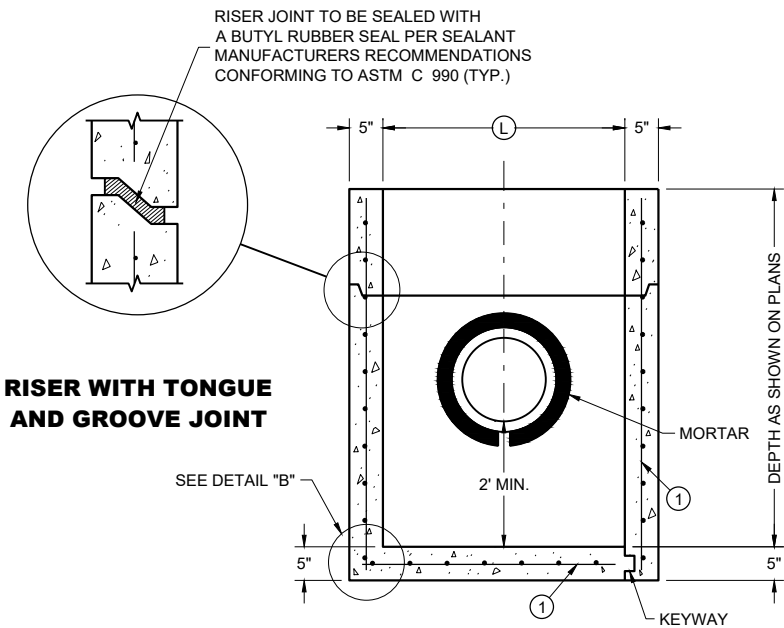
ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

4" OVERHANGING BASES ARE REQUIRED FOR CAST-IN-PLACE REINFORCED CONCRETE AND CONCRETE BLOCK INSTALLATIONS. 4" OVERHANG IS REQUIRED WHEN SEPARATE PRECAST BASE IS PROVIDED. OVERHANG IS NOT REQUIRED ON PRECAST STRUCTURES WITH AN INTEGRAL OR MONOLITHIC BASE.

MAXIMUM INSIDE PIPE DIAMETER DETERMINED BY 3" CLEARANCE ON EACH SIDE OF THE OUTSIDE WALL OF THE PIPE. SEE DETAIL "A". ASSUMES PIPE ENTERS PERPENDICULAR TO THE STRUCTURE.

- ① FOR PRECAST CATCH BASINS AND REINFORCED CONCRETE BASES PROVIDE REINFORCING STEEL IN ACCORDANCE TO ASTM C 913.
- ② CONTRACTOR TO PROVIDE DRAWING(S) STAMPED BY A PROFESSIONAL ENGINEER FOR STEEL REINFORCING DESIGN FOR CAST-IN-PLACE STRUCTURES.
- ③ 1" CONCRETE KEY POURED AFTER INSTALLATION. 2' SUMP MEASURED FROM TOP OF KEY.

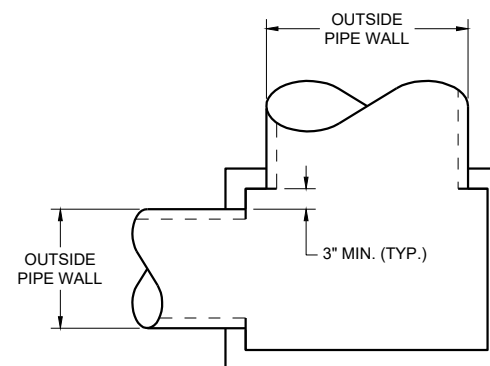
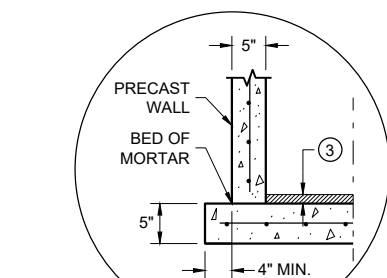


CATCH BASIN COVER MATRIX

CATCH BASIN SIZE	WIDTH (W) (FT.)	LENGTH (L) (FT.)	INLET COVER TYPE	
			F	ALL H'S
2 X 3-FT	2	3		X
2.5 X 3-FT	2.5	3	X	

PIPE MATRIX

CATCH BASIN SIZE	MAXIMUM INSIDE PIPE DIAMETER (IN)	
	WIDTH (W) (IN)	LENGTH (L) (IN)
2 X 3-FT	12	24
2.5 X 3-FT	18	24

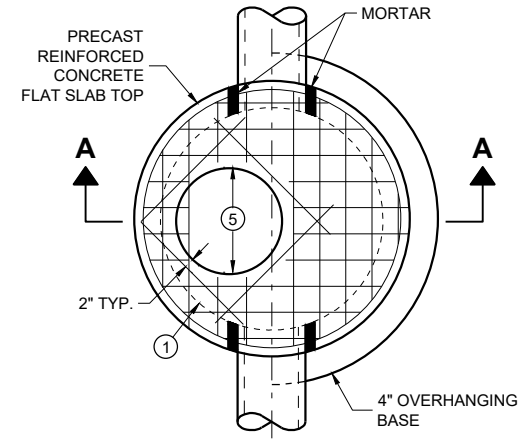


CATCH BASINS 2X3-FT AND 2.5X3-FT

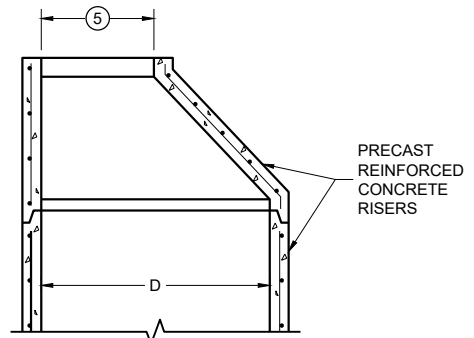
**CATCH BASINS
2 X 3-FT AND 2.5 X 3-FT**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

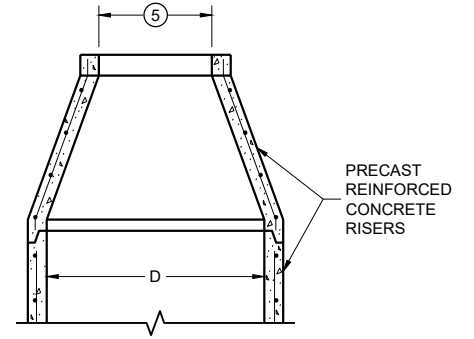
APPROVED
December 2023 DATE /S/ RODNEY TAYLOR
ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR



**PLAN VIEW
CIRCULAR OPENING**



**OPTIONAL PRECAST
REINFORCED CONCRETE
ECCENTRIC TOP**



**OPTIONAL PRECAST
REINFORCED CONCRETE
CONCENTRIC TOP**

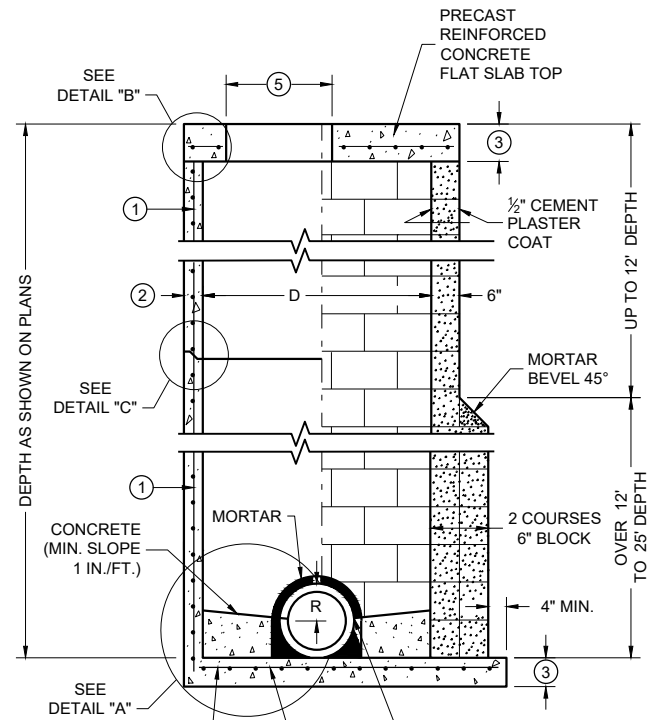
MANHOLE COVER OPENING MATRIX

MANHOLE COVER OPENING SIZE (FT.)	C	ALL J'S	K	L	M
2 DIA.	X	X		X	
3 DIA.			X		X

PIPE MATRIX

MANHOLE SIZE (DIA.)	MAXIMUM INSIDE PIPE DIAMETER FOR TWO PIPES		MINIMUM WALL THICKNESS (IN)	MINIMUM PRECAST FLAT SLAB TOP AND BASE THICKNESS
	180° SEPARATION (IN)	90° SEPARATION (IN)		
3-FT	15	12	4	6
4-FT	24	18	4	6
5-FT	36	24	5	8
6-FT	42	36	6	8
7-FT	48	36/42 *	7	8
8-FT	60	42	8	8
9-FT	66	54	9	10
10-FT	72	60	10	10

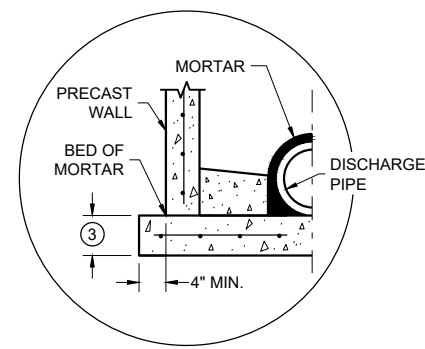
*A 36" PIPE AND A 42" PIPE CAN BE PLACED WITHIN 90 DEGREES. SEE MINIMUM HORIZONTAL PIPE SEPARATION DETAIL.



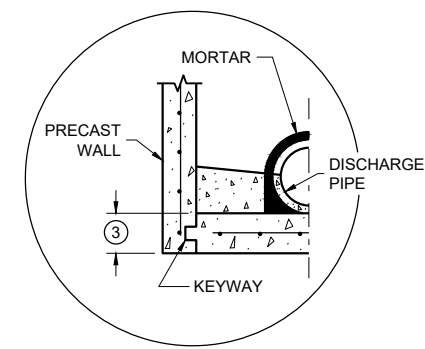
SECTION A - A

**PRECAST REINFORCED
CONCRETE WITH
MONOLITHIC BASE**

**CONCRETE BLOCK WITH
CAST IN PLACE OR
PRECAST REINFORCED
CONCRETE BASE**

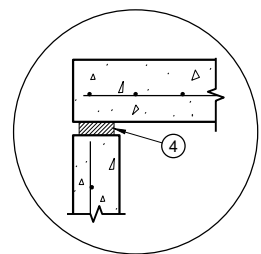


**SEPARATE PRECAST REINFORCED
CONCRETE BASE OPTION**

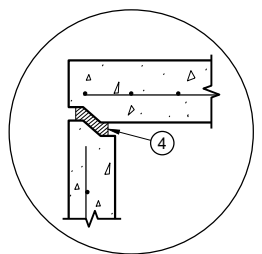


**PRECAST REINFORCED CONCRETE
WITH INTEGRAL BASE OPTION**

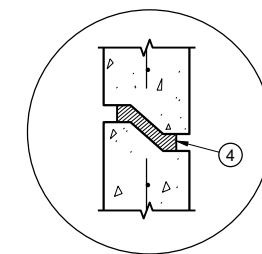
DETAIL "A"



**TOP WITH PLAIN
END JOINT**



**TOP WITH TONGUE
AND GROOVE JOINT**



**RISER WITH TONGUE
AND GROOVE JOINT**

DETAIL "B"

DETAIL "C"

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST MANHOLE UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL DRAINAGE STRUCTURES ARE DESIGNATED ON THE PLANS AS "MANHOLES 3X3-L", "CATCH BASINS 4-B", "INLETS 2X3-H", ETC. THE FIRST NUMBERS DESIGNATE THE SIZE OF THE STRUCTURE, AND THE FOLLOWING LETTER DESIGNATES THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF FOUNDATION BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

PRECAST REINFORCED CONCRETE CONE TOPS (ECCENTRIC OR CONCENTRIC) OR PRECAST REINFORCED CONCRETE FLAT SLAB TOPS MAY BE USED ON CONCRETE BLOCK STRUCTURES.

ECCENTRIC CONE TOPS MAY BE USED ON ALL STRUCTURES. CONCENTRIC CONE TOPS SHALL BE USED ONLY ON STRUCTURES 5 FEET OR LESS IN DEPTH UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

STEPS MEETING AASHTO M199 AND THE FOLLOWING REQUIREMENTS SHALL BE INSTALLED IN ALL STRUCTURES OVER 5 FEET IN DEPTH: 16 INCH C-C MAXIMUM SPACING; PROJECT A MINIMUM CLEAR DISTANCE OF 4 INCHES FROM THE WALL AT THE POINT OF EMBEDMENT; MINIMUM LENGTH OF 10 INCHES; MINIMUM WALL EMBEDMENT OF 3 INCHES. FERROUS METAL STEPS NOT PAINTED OR TREATED TO RESIST CORROSION SHALL HAVE A MINIMUM CROSS SECTIONAL DIMENSION OF 1 INCH.

STEPS OF APPROVED POLYPROPYLENE PLASTIC COATED REINFORCEMENT BAR ARE ACCEPTABLE. REINFORCING BAR MUST BE A MINIMUM OF 1/2 INCH AND MEET THE REQUIREMENTS OF ASTM A615.

CERTIFICATION SHALL BE PROVIDED THAT INSTALLED STEPS WHEN TESTED IN ACCORDANCE WITH SECTION 10 OF AASHTO T280 CAN WITHSTAND A VERTICAL LOAD OF 800 LBS. AND A HORIZONTAL LOAD OF 400 LBS.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

ALL PRECAST MANHOLE UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF AASHTO DESIGNATION M199.

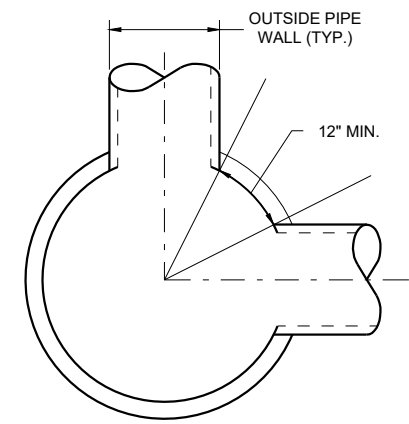
PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

CONCRETE BLOCK WILL NOT BE PERMITTED FOR STRUCTURES GREATER THAN 4 FEET IN DIAMETER.

4" OVERHANGING BASES ARE REQUIRED FOR ALL CONCRETE BLOCK INSTALLATIONS. 4" OVERHANG IS REQUIRED WHEN SEPARATE PRECAST BASE IS PROVIDED. OVERHANG IS NOT REQUIRED ON PRECAST STRUCTURES WITH AN INTEGRAL OR MONOLITHIC BASE.

FOR ADDITIONAL CONFIGURATIONS, MAINTAIN A MINIMUM OF 12 INCHES AS MEASURED FROM THE INSIDE OF THE STRUCTURE WALL BETWEEN THE OUTSIDE PIPE WALLS OF ADJACENT PIPES. SEE DETAIL "D".

- ① FOR PRECAST MANHOLES AND REINFORCED CONCRETE BASES PROVIDE REINFORCING STEEL IN ACCORDANCE TO AASHTO M199.
- ② SEE PIPE MATRIX TABLE FOR MINIMUM WALL THICKNESS FOR PRECAST MANHOLES
- ③ SEE PIPE MATRIX TABLE FOR MINIMUM THICKNESS OF PRECAST FLAT SLAB TOPS AND BASES.
- ④ JOINTS TO BE SEALED WITH A BUTYL RUBBER SEAL PER SEALANT MANUFACTURERS RECOMMENDATIONS CONFORMING TO ASTM C 990 OR RUBBER GASKETS CONFORMING TO ASTM C443.
- ⑤ SEE MANHOLE COVER OPENING MATRIX.



**MINIMUM HORIZONTAL
PIPE SEPARATION
DETAIL "D"**

**MANHOLES, 3-FT, 4-FT
5-FT, 6-FT, 7-FT, 8-FT, 9-FT
AND 10-FT DIAMETER**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
December 2023 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
UNIT SUPERVISOR
FHWA

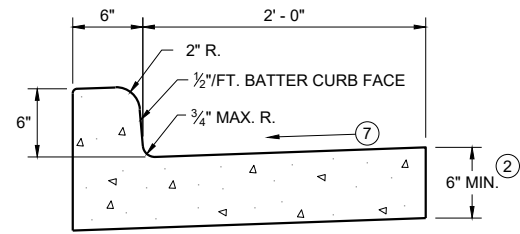
6

6

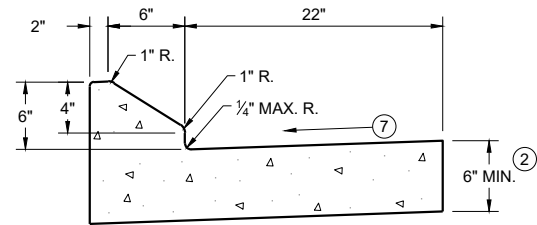
SDD 08B09-04

SDD 08B09-04

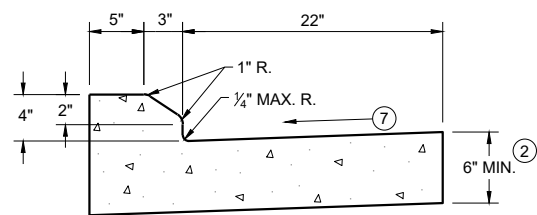
MANHOLES 3-FT, 4-FT, 5-FT, 6-FT, 7-FT, 8-FT, 9-FT AND 10-FT DIAMETER



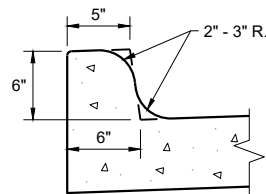
TYPES A¹ & D



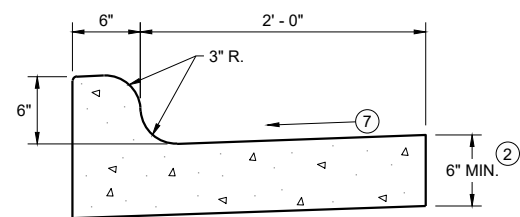
6" SLOPED CURB TYPES G¹ & J



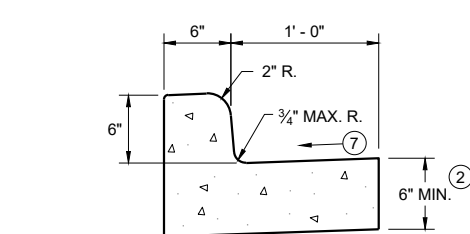
4" SLOPED CURB TYPES G¹ & J



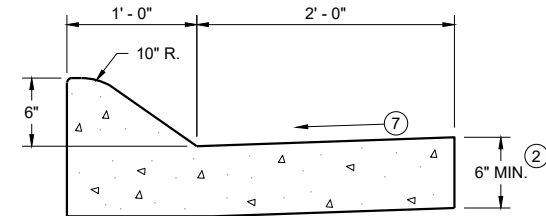
TYPES K¹ & L
(OPTIONAL CURB SHAPE)



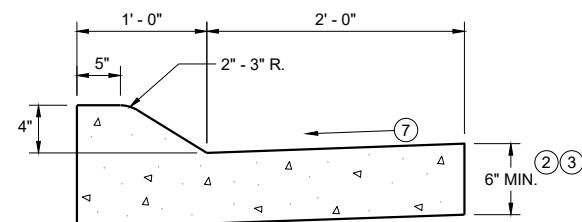
TYPES K¹ & L
CONCRETE CURB AND GUTTER 30"



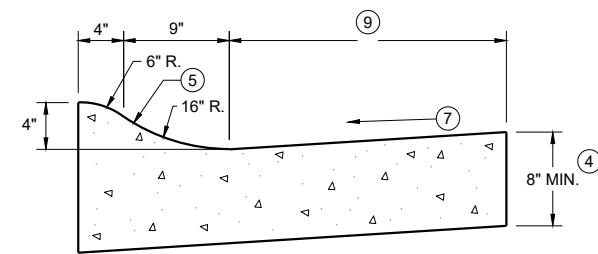
TYPES A¹ & D
CONCRETE CURB AND GUTTER 18"



6" SLOPED CURB TYPES A¹ & D

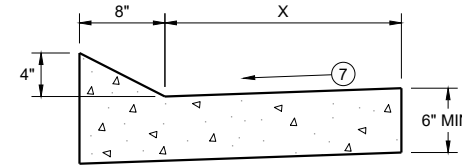


4" SLOPED CURB TYPES A¹ & D
CONCRETE CURB AND GUTTER 36"



4" SLOPED CURB TYPES R¹ & T

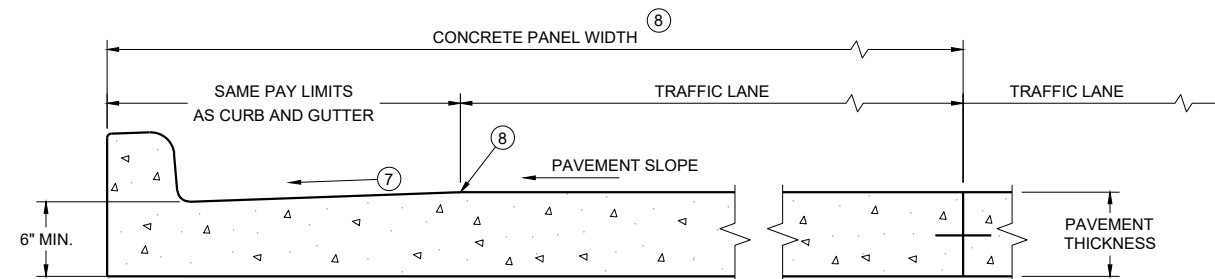
TBT & TBTT	X
30"	22"
36"	28"



TYPES TBT & TBTT¹
CONCRETE CURB AND GUTTER

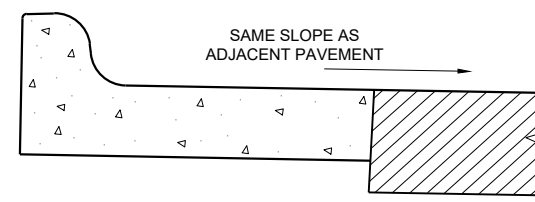
PAVEMENT THICKNESS AND MAXIMUM CONCRETE PANEL WIDTH TABLE

PAVEMENT THICKNESS	MAXIMUM PANEL WIDTH
LESS THAN 10"	12'
10" & ABOVE	15'



PARTIAL SECTION OF PAVEMENT WITH INTEGRAL CURB AND GUTTER

* BIKE LANE IS NOT SHOWN



REVERSE SLOPE GUTTER⁶
(TYPICAL FOR ALL CURB & GUTTER TYPES)

GENERAL NOTES

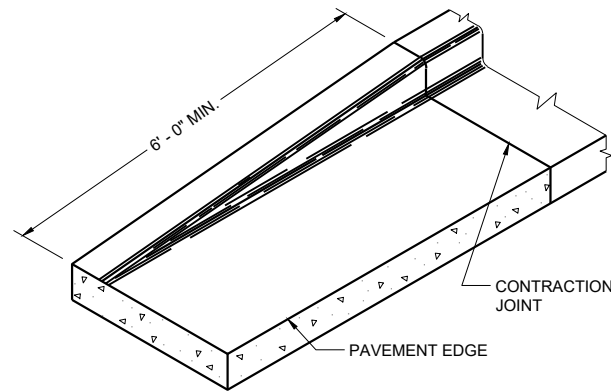
DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

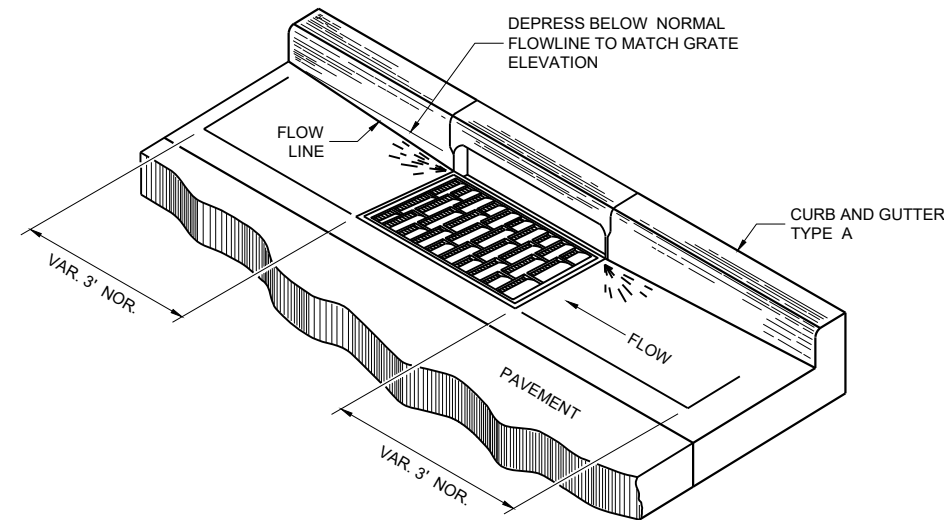
INTEGRAL CURB AND GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB AND GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ③ USE 8" MINIMUM GUTTER THICKNESS WHEN USED WITH AN ADJACENT CONCRETE TRUCK APRON PLACED BEHIND BACK OF CURB.
- ④ THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑤ UNLESS OTHERWISE NOTED, FOR STAKING PURPOSES THE FACE OF CURB IS 6" FROM THE BACK OF CURB.
- ⑥ WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.
- ⑦ USE 4% GUTTER CROSS SLOPE UNLESS OTHERWISE NOTED IN THE PLANS.
- ⑧ INCLUDE LONGITUDINAL JOINT AND TIE BARS ALONG LANE EDGE WHEN CONCRETE PANEL WIDTH EXCEEDS THE MAXIMUM WIDTH PER TABLE BELOW. LONGITUDINAL JOINT(S) ARE NOT ALLOWED WITHIN TRAFFIC LANES AND BIKE LANES. LONGITUDINAL JOINT MAY BE SAWED.
- ⑨ CONCRETE CURB AND GUTTER 4-INCH SLOPED 30-INCH TYPE "R" AND "T" = 17 INCHES
CONCRETE CURB AND GUTTER 4-INCH SLOPED 36-INCH TYPE "R" AND "T" = 23 INCHES



END SECTION CURB AND GUTTER



DETAIL OF CURB AND GUTTER AT INLETS

(TYPICAL H INLET COVER SHOWN)

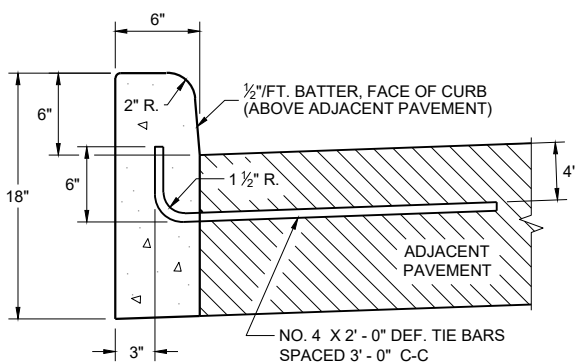
GENERAL NOTES

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

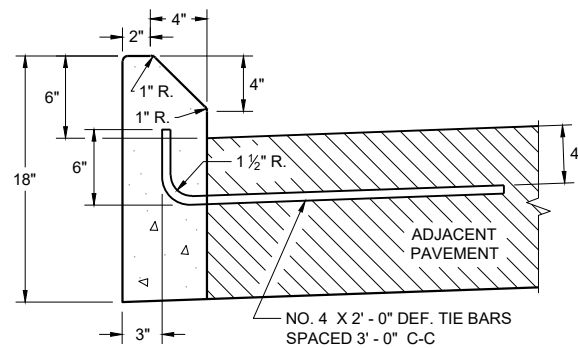
PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

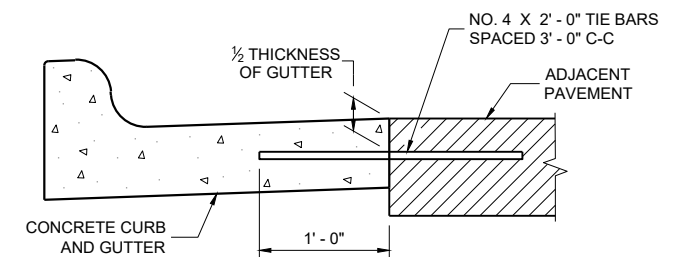
- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑩ REFER TO SDD 08D18 AND 08D19 FOR ADDITIONAL DRIVEWAY ENTRANCE CURB DETAILS.
- ⑪ PLACE 1" THICK EXPANSION JOINT MATERIAL BETWEEN VERTICAL FACE CURB TYPES EXTENDING FROM THE TOP OF CURB TO 1 INCH BELOW THE ADJOINING CONCRETE SURFACE. RIGID CONCRETE STRUCTURES INCLUDE RAISED CONCRETE MEDIANS, CONCRETE SAFETY ISLANDS, SPLITTER ISLANDS, OR LOCATIONS IDENTIFIED ON THE PLANS.



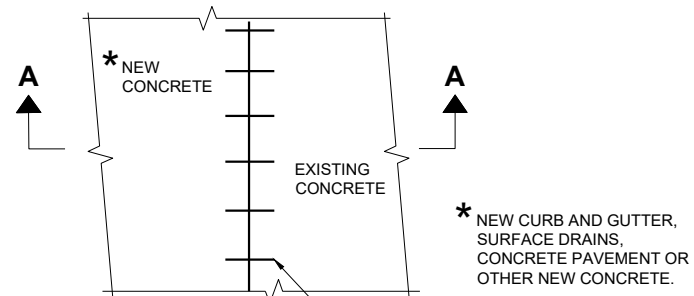
TYPES A ① & D



**TYPES G ① & J
CONCRETE CURB**

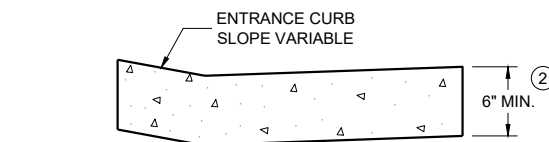


TYPICAL TIE BAR LOCATION ①

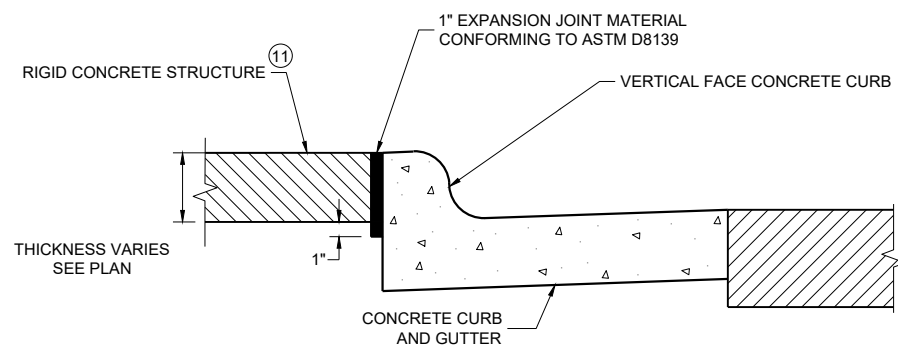


PLAN VIEW

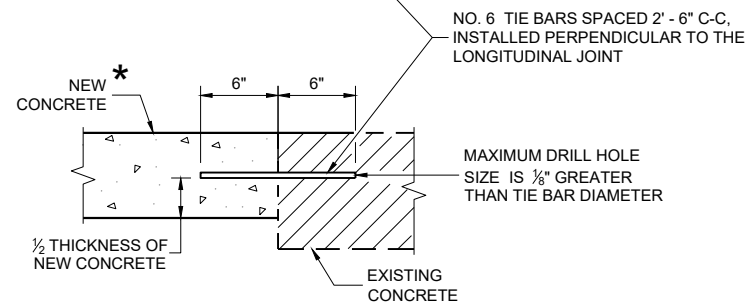
* NEW CURB AND GUTTER, SURFACE DRAINS, CONCRETE PAVEMENT OR OTHER NEW CONCRETE.



DRIVEWAY ENTRANCE CURB ⑩
(WHEN DIRECTED BY THE ENGINEER)



EXPANSION JOINT DETAIL FOR VERTICAL CURB ABUTTING A RIGID STRUCTURE ⑪



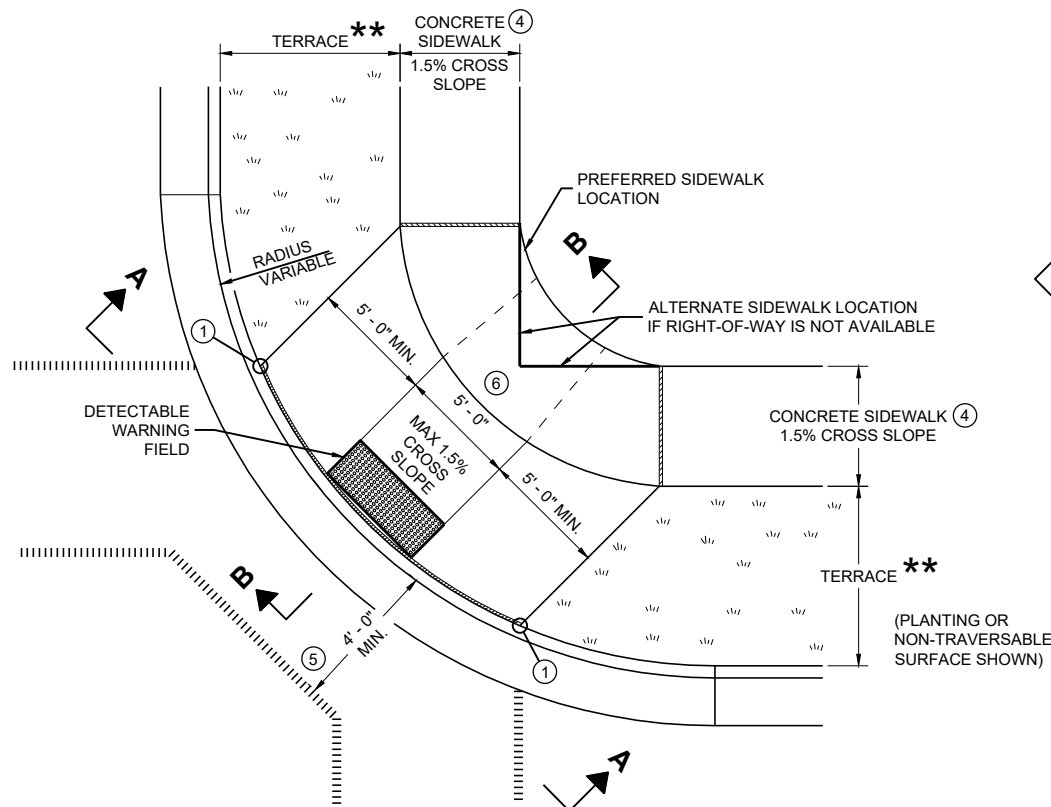
SECTION A - A

TIE BARS DRILLED INTO EXISTING PAVEMENT

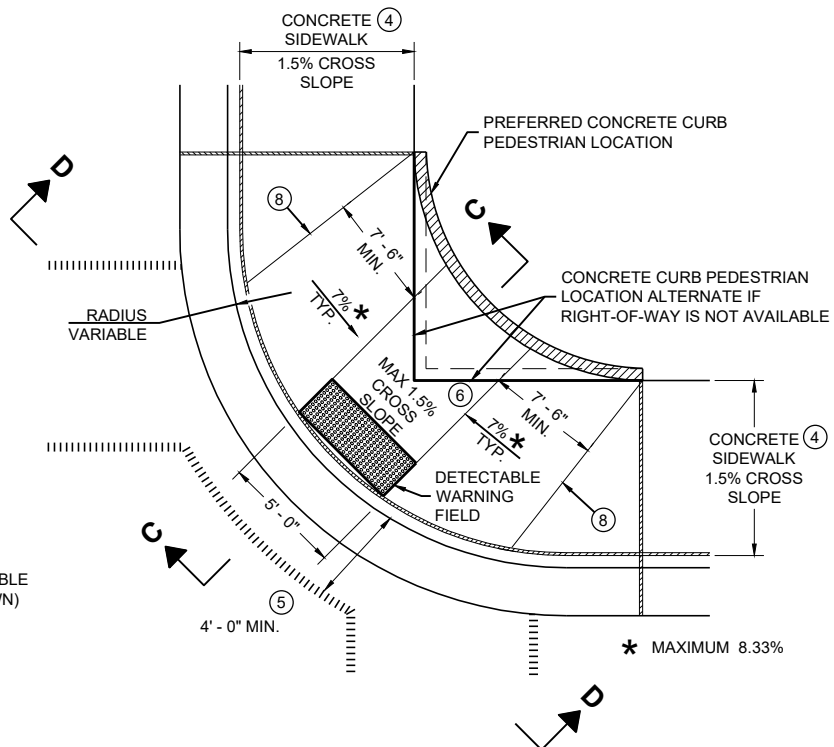
CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

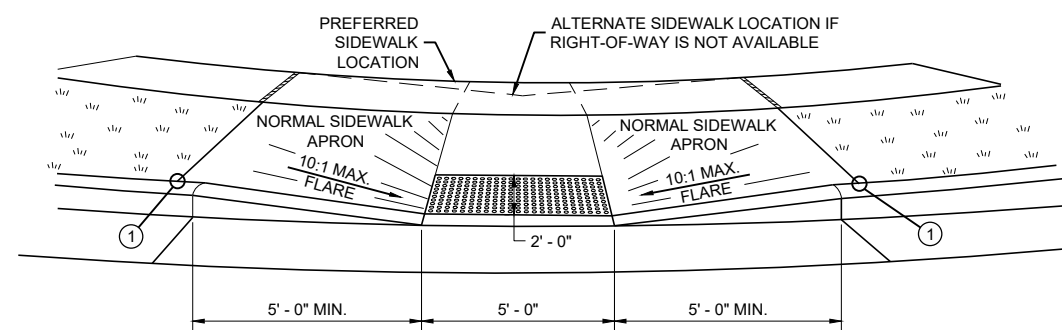
APPROVED
May 2023 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
FHWA UNIT SUPERVISOR



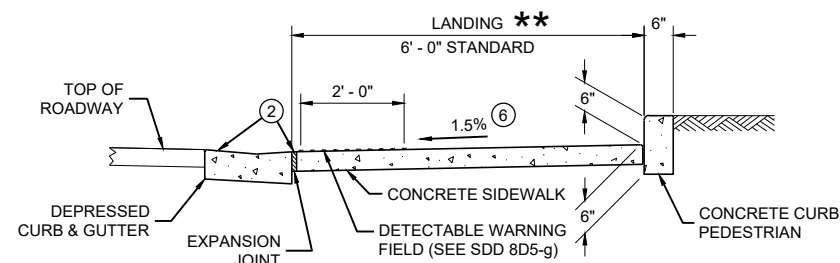
**PLAN VIEW
CURB RAMP TYPE 1
(CENTER OF CORNER RADIUS)**



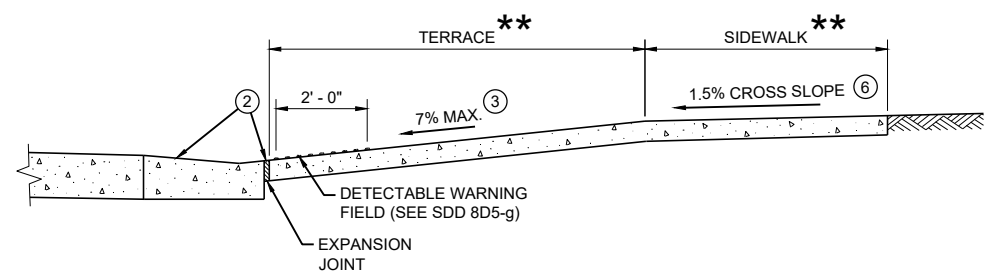
**PLAN VIEW
CURB RAMP TYPE 1 - A
(NO TERRACE)**



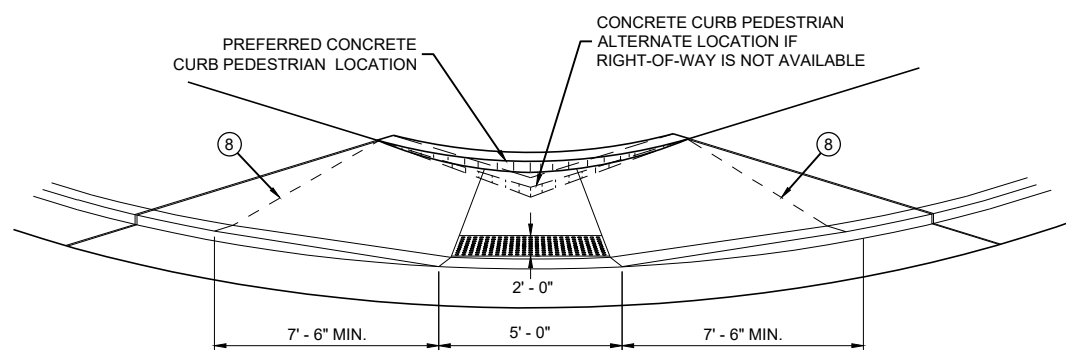
VIEW A - A FOR TYPE 1



SECTION C - C FOR TYPE 1 - A



SECTION B - B FOR TYPE 1



VIEW D - D FOR TYPE 1 - A

GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- WHEN NECESSARY, THE SIDEWALK ELEVATION MAY BE LOWERED TO MEET THE HIGH POINT ON THE RAMP.
- TYPE 1 CURB RAMPS SHALL HAVE A NORMAL SIDEWALK APRON AND CURB ON BOTH SIDES OF RAMP.
- DETECTABLE WARNING FIELD SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS "CURB RAMP DETECTABLE WARNING FIELD". THE CONCRETE PEDESTRIAN CURB, IF NEEDED, SHALL BE MEASURED AND PAID BY THE LINEAR FOOT AS "CONCRETE CURB PEDESTRIAN". CONCRETE SIDEWALK IN THE CURB RAMP AREA SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS CONCRETE SIDEWALK, INCLUDING THE AREA UNDER THE DETECTABLE WARNING FIELD.
- SELECT CURB RAMP DETECTABLE WARNING FIELD MATERIALS AND DEVICES FROM THE DEPARTMENT'S APPROVED MATERIALS LIST. THE COLOR OF THE DETECTABLE WARNING FIELD IS SPECIFIED ELSEWHERE AND IS INCIDENTAL TO THE BID ITEM OF "CURB RAMP DETECTABLE WARNING FIELD"
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- SURFACE TEXTURE OF THE RAMP SHALL BE OBTAINED BY COARSE BROOMING TRANSVERSE TO THE SLOPE OF THE RAMP.
- ① THIS POINT IS AN EXTENSION OF OUTSIDE EDGE OF APPROACHING SIDEWALK WHERE IT MEETS THE BACK OF CONCRETE CURB. POINT LOCATION MAY BE ADJUSTED TO ALIGN WITH BEGINNING OF FULL-HEIGHT CURB IF THIS DISTANCE IS SHORT.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ MAXIMUM 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑤ PROVIDE A LEVEL LANDING IN THE STREET AND GUTTER AREA (2% MAXIMUM SLOPE IN ANY DIRECTION). WHEN THE GUTTER SLOPE EXCEEDS 2%, CONSTRUCT THE LEVEL LANDING IN THE STREET AREA. 4 FOOT WIDTH IS MEASURED FROM THE FLANGE LINE
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

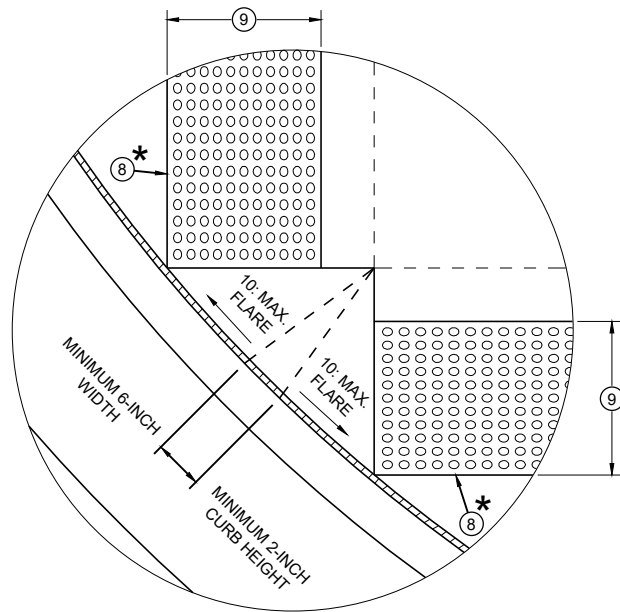
LEGEND

- 1/2" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)

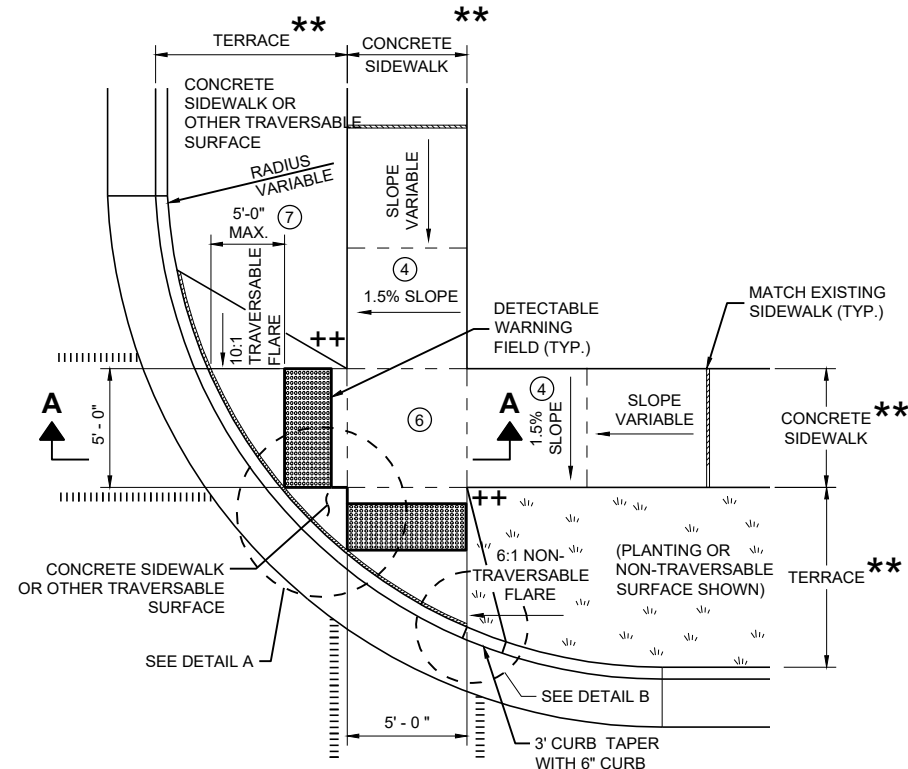
**CURB RAMPS
TYPE 1 AND 1-A**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

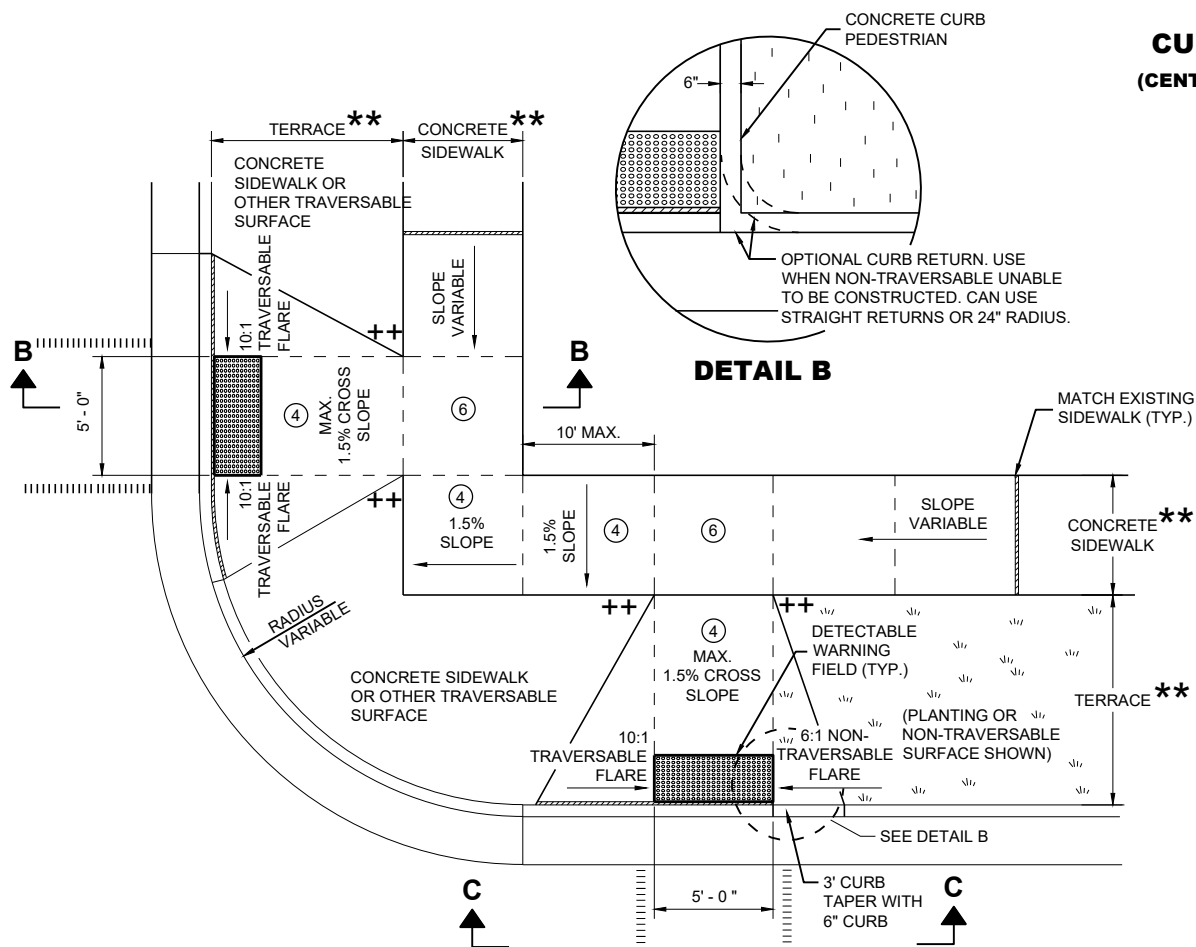
** WIDTH SHOWN ELSEWHERE
IN THE PLANS



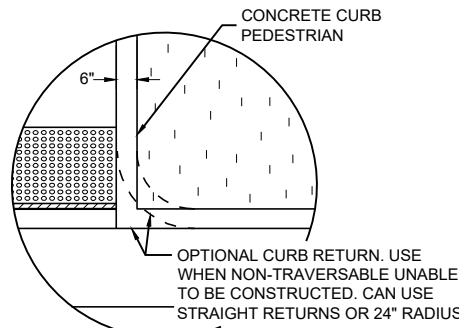
DETAIL A



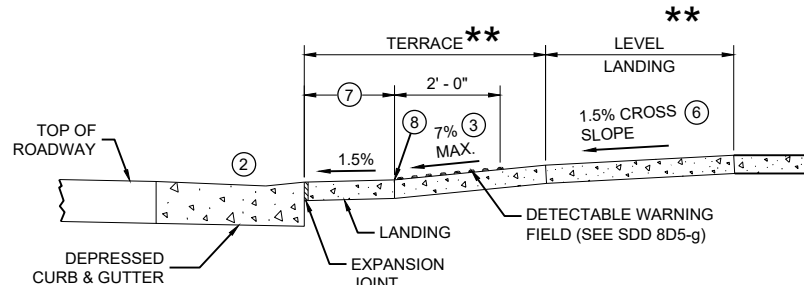
**PLAN VIEW
CURB RAMP TYPE 2
(CENTER OF CORNER RADIUS)**



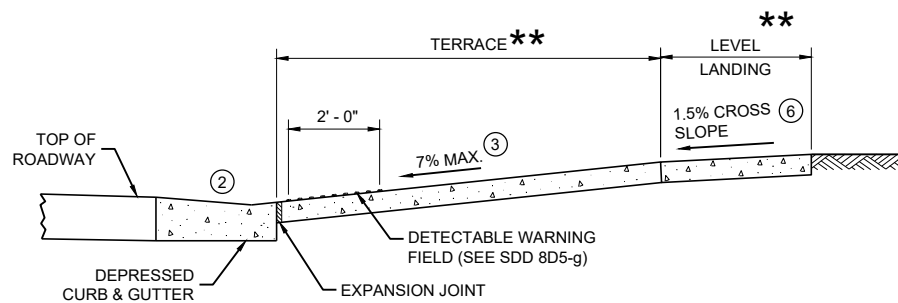
**PLAN VIEW
CURB RAMP TYPE 3
(OUTSIDE OF CROSSWALK AREA)**



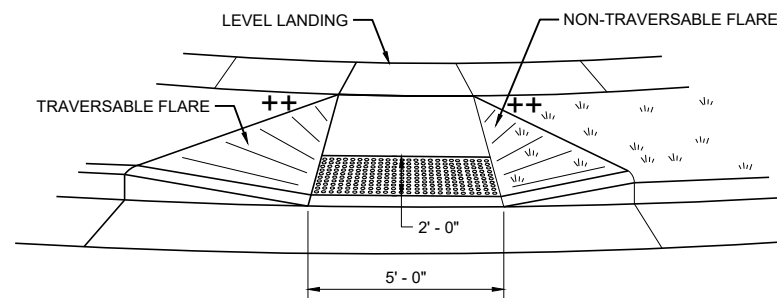
DETAIL B



SECTION A - A FOR TYPE 2



SECTION B - B FOR TYPE 3



VIEW C - C FOR TYPE 3

GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE (2.67% OR LESS) AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET X 5 FEET.
- ⑦ WHEN GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑨ WHEN DISTANCE IS LESS THAN 6' - 0", IT MAY BE DIFFICULT TO ACHIEVE A 7% SLOPE OR FLATTER ALONG THE RAMP. REDUCE CURB HEIGHT IN TRIANGLE AREA TO ACHIEVE 7% SLOPE OR FLATTER ON RAMP. CONSTRUCT 2-INCH MINIMUM CURB HEIGHT BETWEEN 10:1 FLARES.

★ MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK

★★ WIDTH SHOWN ELSEWHERE IN THE PLANS

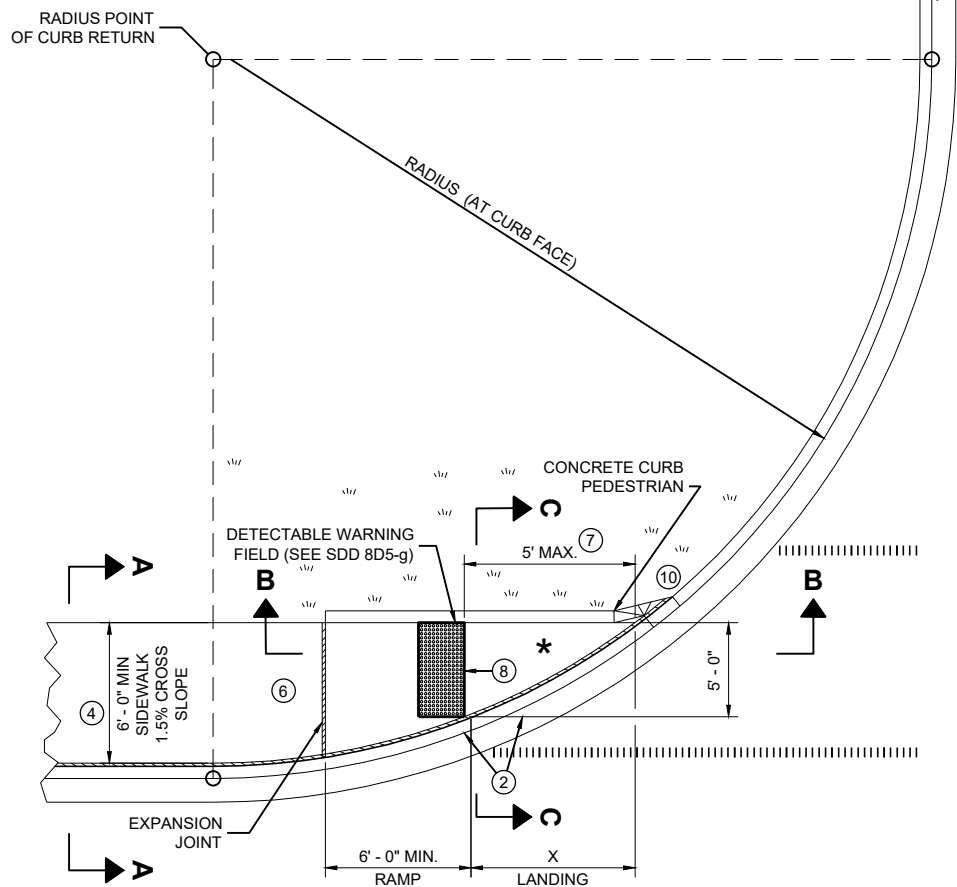
+++ CONSTRUCT 6" WEDGE TO AVOID CONCRETE BREAKAGE

LEGEND

- 1/2" EXPANSION JOINT SIDEWALK
- - - - - CONTRACTION JOINT SIDEWALK
- ||||| PAVEMENT MARKING CROSSWALK (WHITE)

**CURB RAMPS
TYPE 2 AND 3**

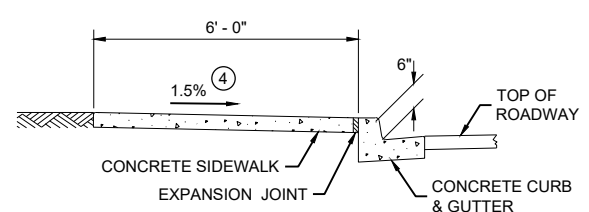
STATE OF WISCONSIN
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**PLAN VIEW
CURB RAMP TYPE 4A**

RADIUS (AT CURB FACE)	X
10 FEET	4' - 7"

INTERMEDIATE RADII CAN BE INTERPOLATED



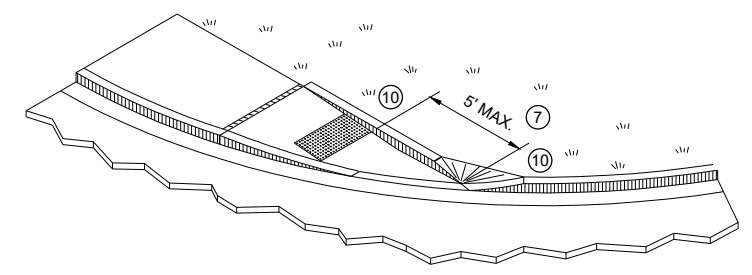
SECTION A - A FOR TYPE 4A

GENERAL NOTES

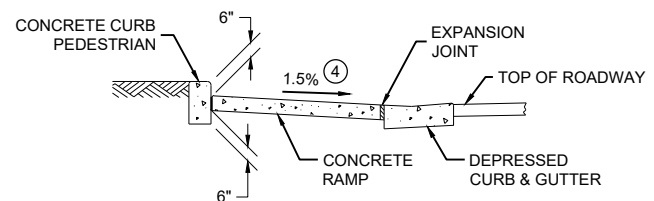
- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑦ WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑩ INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

LEGEND

- 1/2" EXPANSION JOINT SIDEWALK
- - - - CONTRACTION JOINT SIDEWALK
- ||||| PAVEMENT MARKING CROSSWALK (WHITE)

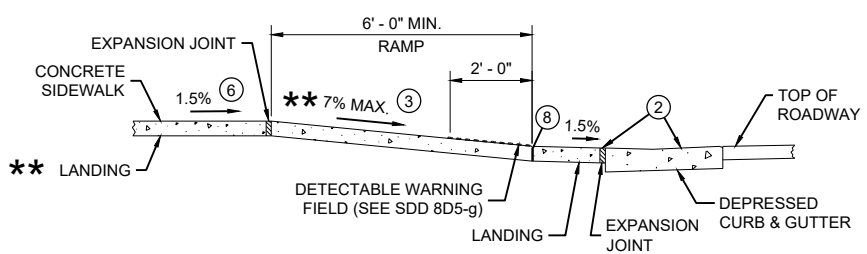


ISOMETRIC VIEW FOR TYPE 4A



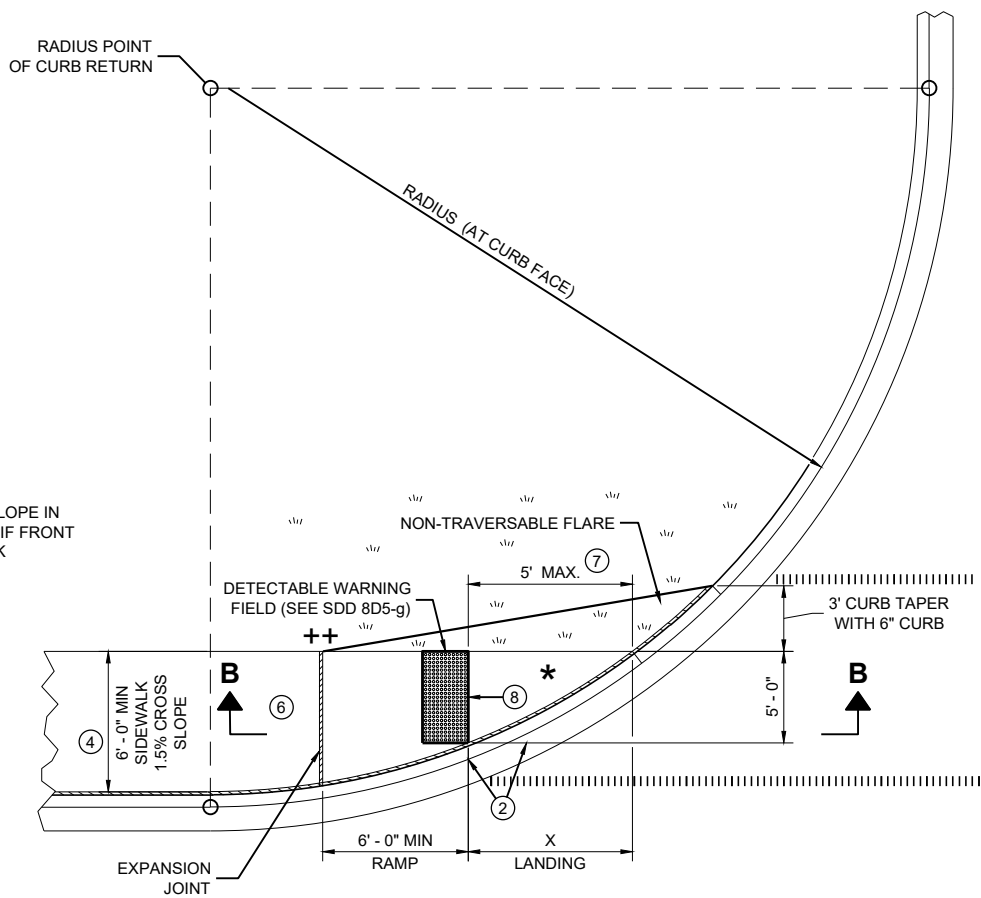
SECTION C - C FOR TYPE 4A

* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IF FRONT OF GRADE BREAK



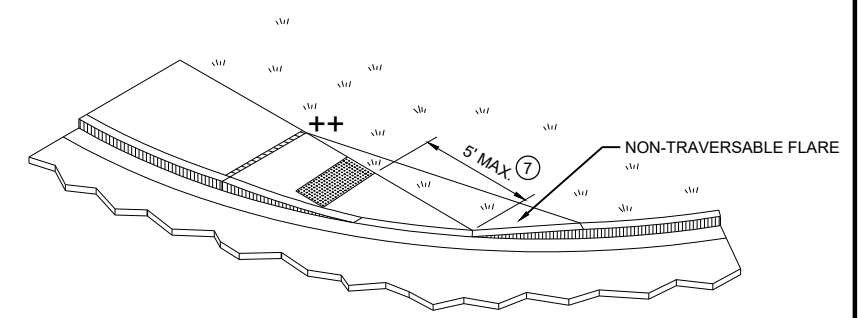
**SECTION B - B FOR
TYPE 4A AND TYPE 4A1**

** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED



**PLAN VIEW
CURB RAMP TYPE 4A1**

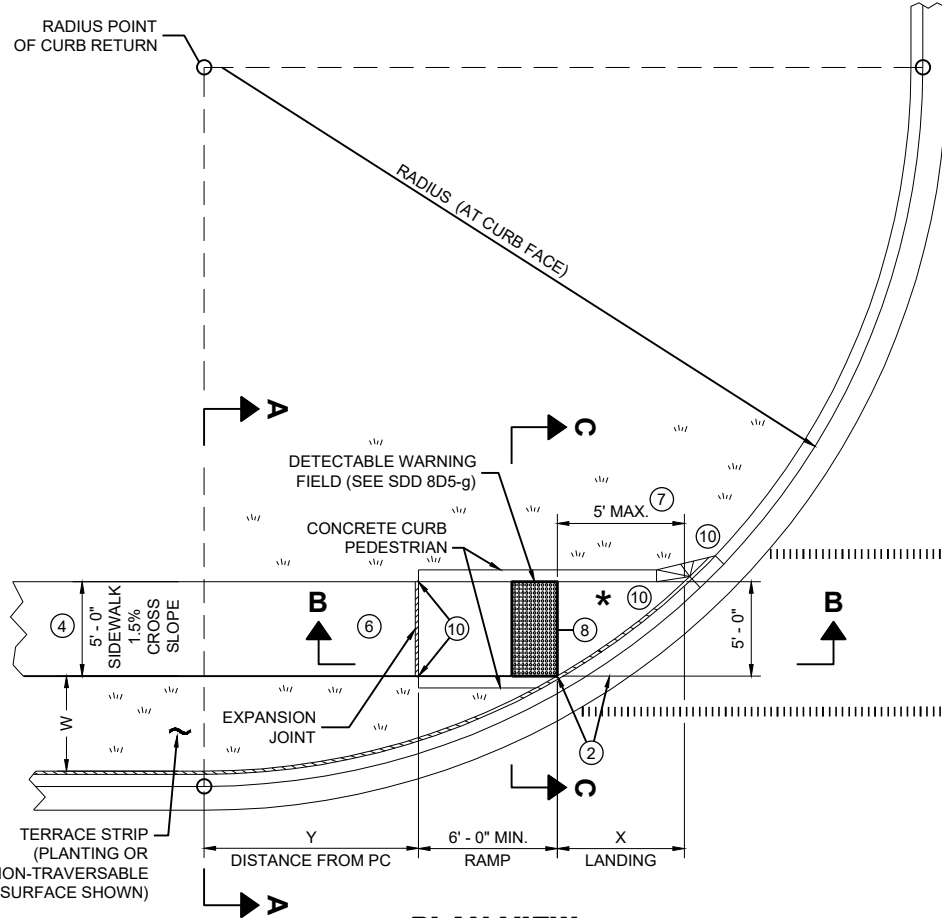
++ CONSTRUCT 6" WEDGE TO AVOID CONCRETE BREAKAGE



ISOMETRIC VIEW FOR TYPE 4A1

**CURB RAMPS
TYPE 4A AND 4A1**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



RADIUS (AT CURB FACE)	W = 3' - 0"		W = 4' - 0"		W = 5' - 0"		W = 6' - 0"		W = 7' - 0"		W = 8' - 0"		W = 9' - 0"		W = 10' - 0"	
	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y
10 FEET	2' - 10 1/4"	0' - 5"	2' - 1"	1' - 4 1/2"	1' - 5"	2' - 1"	0' - 10"	2' - 7 1/2"	0' - 3 1/4"	3' - 0 1/4"						
15 FEET	4' - 6 3/4"	2' - 1 3/4"	3' - 9"	3' - 5 3/4"	3' - 1 1/4"	4' - 6"	2' - 6 3/4"	5' - 4 1/2"	2' - 1"	6' - 1"	1' - 8"	6' - 8 1/2"	1' - 3 1/4"	7' - 2 1/2"	0' - 10 3/4"	7' - 7 1/4"
20 FEET			4' - 11 1/2"	5' - 1 3/4"	4' - 3 1/4"	6' - 5 1/2"	3' - 8 3/4"	7' - 7"	3' - 3"	8' - 6 1/2"	2' - 10"	9' - 4 1/2"	2' - 5 1/2"	10' - 1 1/4"	2' - 1 1/4"	10' - 9"
30 FEET									4' - 10 3/4"	12' - 5 3/4"	4' - 5 1/2"	13' - 7 3/4"	4' - 0 3/4"	14' - 8 1/2"	3' - 8 1/2"	15' - 8 1/4"
40 FEET															4' - 10 3/4"	19' - 8 1/4"

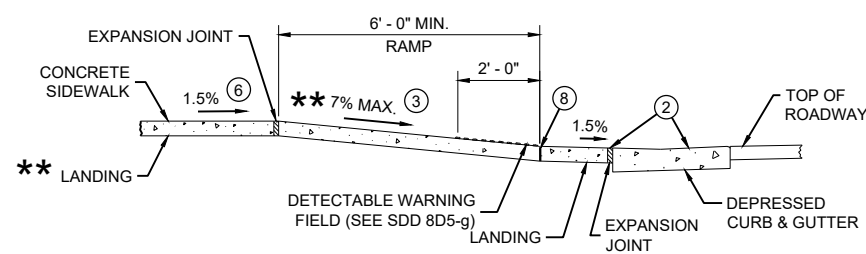
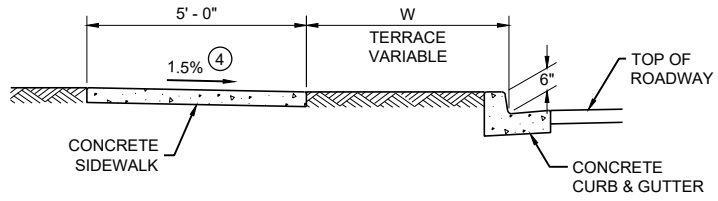
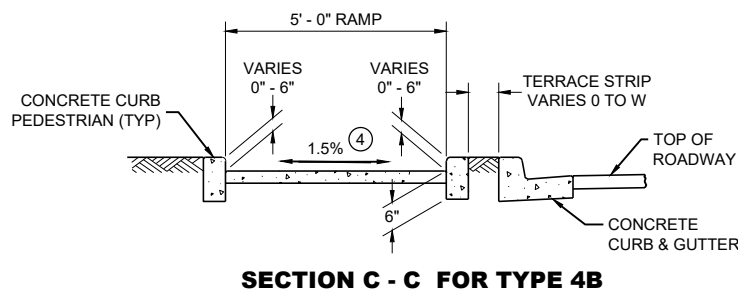
LEGEND

- 1/2" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT SIDEWALK
- PAVEMENT MARKING CROSSWALK (WHITE)

INTERMEDIATE RADII CAN BE INTERPOLATED
 DIMENSION "Y" IS CALCULATED BASED ON 6'-0" RAMP LENGTH
 DIMENSION "X" IS CALCULATED BASED ON 5'-0" SIDEWALK WIDTH

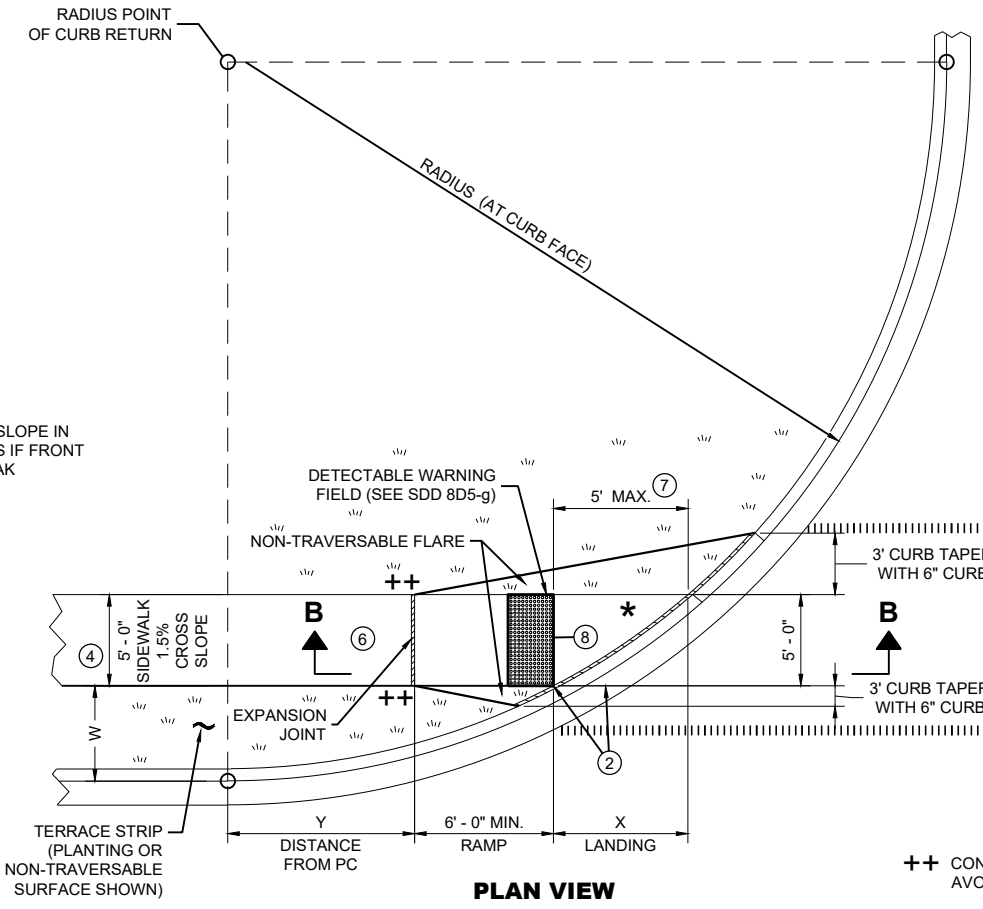
GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4" - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑦ WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑩ INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

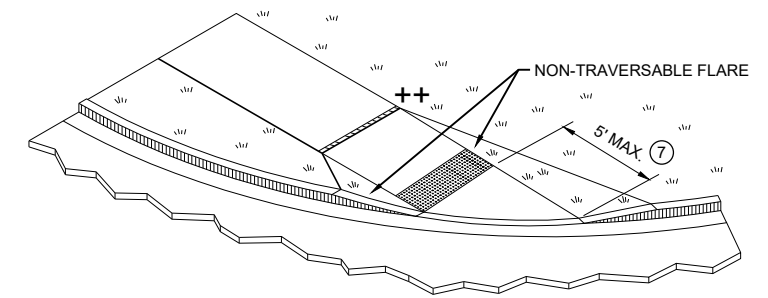
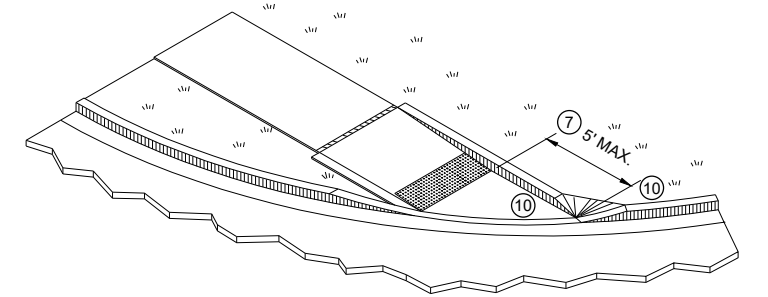


** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IF FRONT OF GRADE BREAK



++ CONSTRUCT 6" WEDGE TO AVOID CONCRETE BREAKAGE



**CURB RAMPS
TYPE 4B AND 4B1**

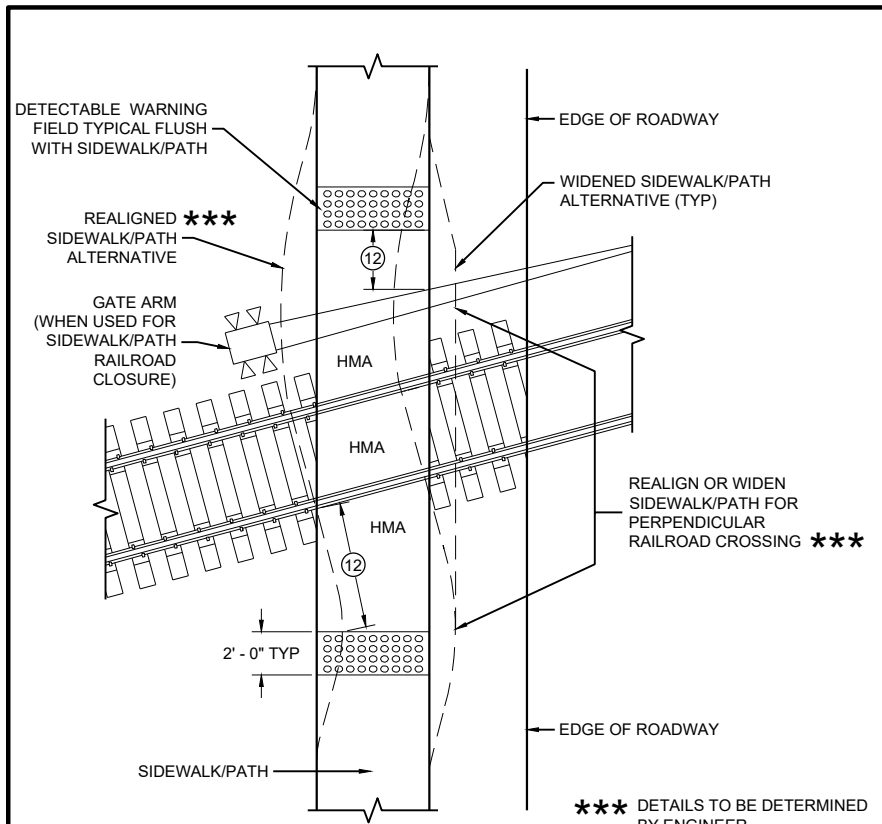
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

6

SDD 08D05-21d

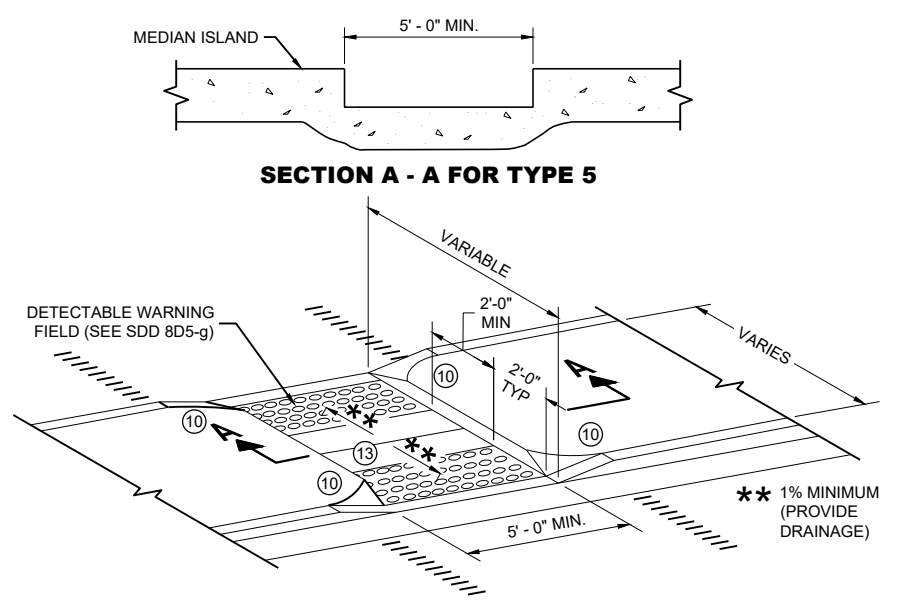
SDD 08D05-21d



CURB RAMP TYPE 8

DETECTABLE WARNINGS FOR SIDEWALKS OR SHARED USE PATHS AT RAILROAD CROSSINGS

*** DETAILS TO BE DETERMINED BY ENGINEER



SECTION A - A FOR TYPE 5

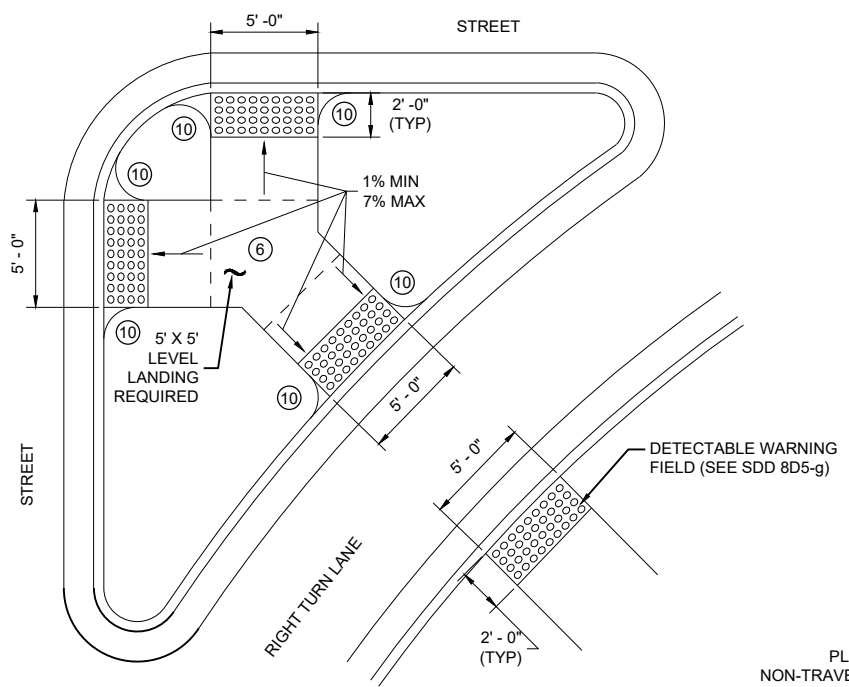
**CURB RAMP TYPE 5
MEDIAN ISLAND
NON-ELEVATED PEDESTRIAN CROSSING**

GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑩ INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.
- ⑪ SLOPE SIDEWALK TOWARD LANDING AS SHOWN WHERE THERE IS NO TERRACE OR WHERE THE TERRACE WIDTH IS LESS THAN 6 FEET WIDE.
- ⑫ THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO A RAILROAD CROSSING SHALL BE 1.5 FEET ±0.1' FROM THE FACE OF THE GATE ARM IF THE GATE ARM EXTENDS ACROSS THE SIDEWALK/PATH. WHERE THERE IS NO PEDESTRIAN GATE, THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO THE RAILROAD TRACK IS 15 FEET MAXIMUM AND 12 FEET MINIMUM, 15 FEET TYPICAL FROM THE NEAREST RAIL.
- ⑬ DO NOT INSTALL DETECTABLE WARNING FIELDS AT THE EDGES OF STREET-LEVEL PEDESTRIAN REFUGE ISLANDS IF A MINIMUM 2 FOOT CONCRETE SURFACE WITHOUT DETECTABLE WARNINGS (MEASURED IN THE DIRECTION OF PEDESTRIAN TRAVEL) CANNOT BE ACHIEVED.

LEGEND

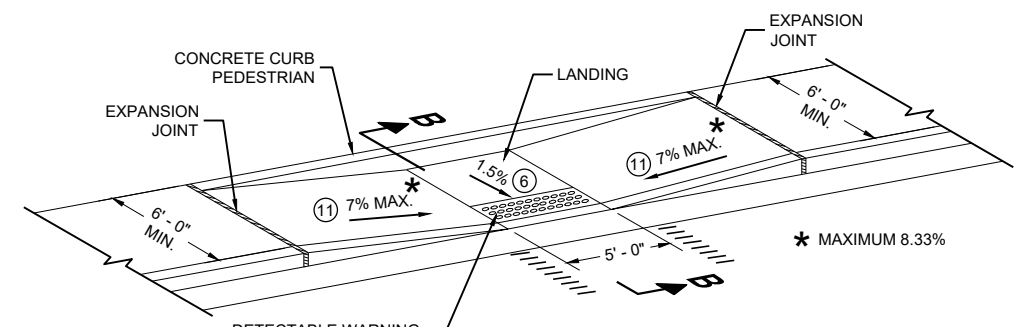
- ===== 1/2" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- ||||||| PAVEMENT MARKING CROSSWALK (WHITE)



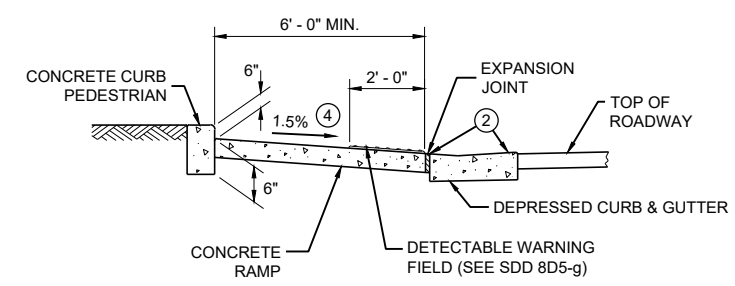
CURB RAMP TYPE 6

DETECTABLE WARNING AT ISLANDS

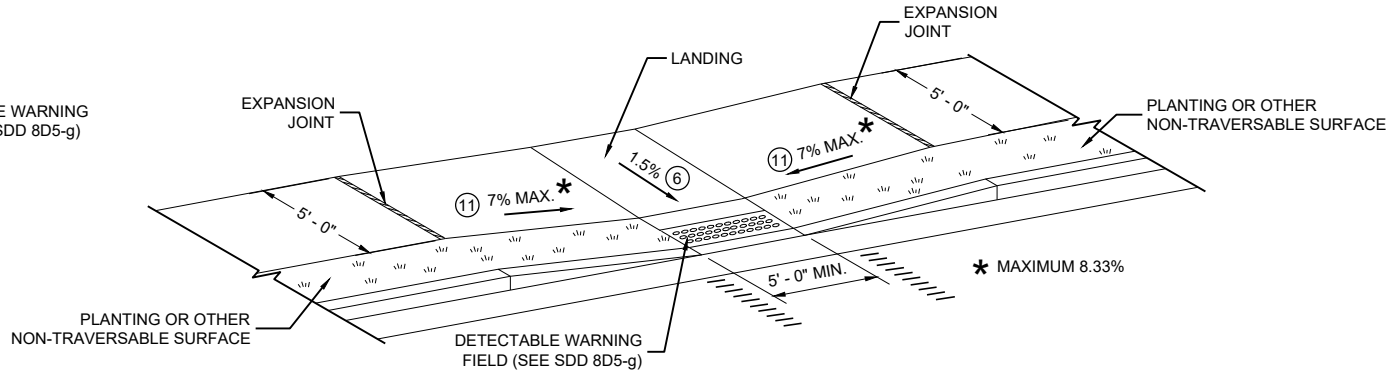
REFER TO GENERAL NOTES ② AND ③ FOR ALL ISLAND CURB RAMPS



**CURB RAMP TYPE 7A
FOR INTERSECTIONS AND
MID BLOCK CROSSINGS**



SECTION B - B FOR TYPE 7A



**CURB RAMP TYPE 7B
FOR INTERSECTIONS AND
MID BLOCK CROSSINGS**

6

6

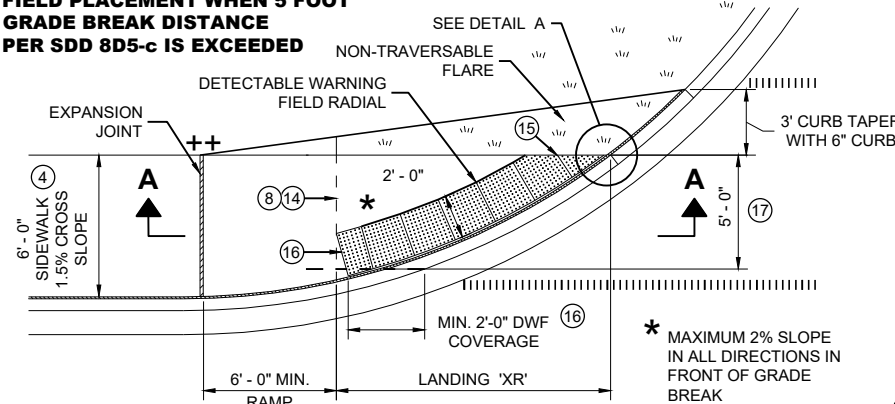
SDD 08D05-21e

SDD 08D05-21e

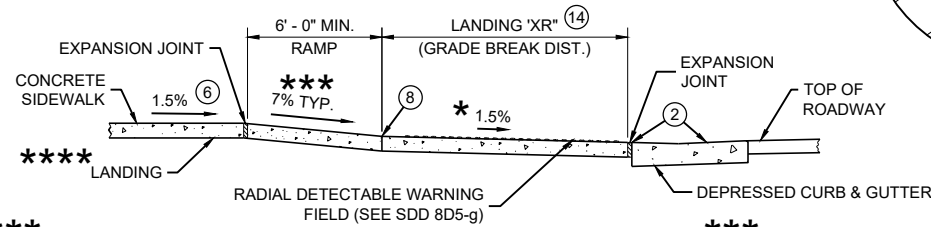
**CURB RAMPS
TYPE 5, 6, 7A, 7B & 8**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-c IS EXCEEDED



**PLAN VIEW
CURB RAMP TYPE 4A1
(GRADE BREAK DISTANCE GREATER THAN 5 FEET)**



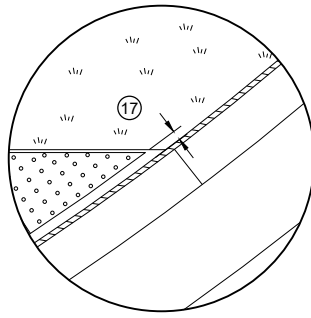
SECTION A - A FOR TYPE 4A1

**** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

*** MAXIMUM 8.33%

LEGEND

- ===== 1/2" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT SIDEWALK
- ||||||| PAVEMENT MARKING CROSSWALK (WHITE)

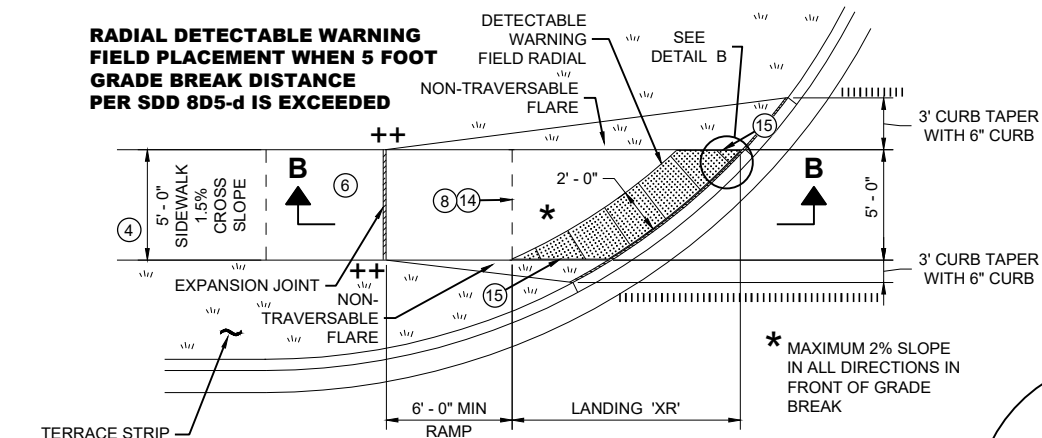


DETAIL A

GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- APPLY RADIAL DETECTABLE WARNING PLACEMENT SIMILARLY FOR TYPE 4A AND 4A1 CURB RAMPS AND SIMILARLY FOR TYPE 4B AND 4B1 CURB RAMPS. TYPE 4A AND 4B CURB RAMPS ARE NOT SHOWN.
- REFER TO SDD 8D5-g FOR ADDITIONAL RADIAL PLATE REQUIREMENTS.
- FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FIELD ARE PROHIBITED.
- DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.
- (2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4" INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- (3) AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- (4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- (6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET BY 5 FEET.
- (8) PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- (14) CONSULT ENGINEER IF GRADE BREAK LOCATION (END OF LANDING DIMENSION "XR") REQUIRES FIELD ADJUSTMENT WHEN ESTABLISHING FINAL RADIAL DETECTABLE WARNING FIELD LOCATION.
- (15) FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN 1/2" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.
- (16) USE 1' X 2" RECTANGULAR END PLATE AT END OF TYPE 4A1 RAMP AND PROVIDE MINIMUM 2' - 0" DETECTABLE WARNING FIELD COVERAGE (IN DIRECTION OF PEDESTRIAN TRAVEL) ALONG THE ENTIRE CURB RAMP WIDTH.
- (17) A MAXIMUM 3 INCH CONCRETE BORDER WIDTH IS ALLOWABLE IN FRONT OF RADIAL DETECTABLE WARNING FIELD FOR CONSTRUCTABILITY PURPOSES. CONCRETE BORDER WIDTH MAY VARY UP TO 1 INCH.

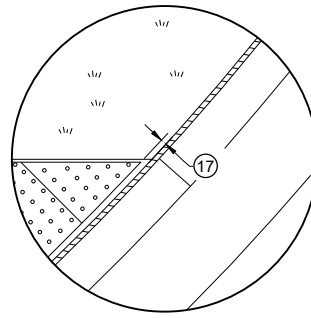
RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-d IS EXCEEDED



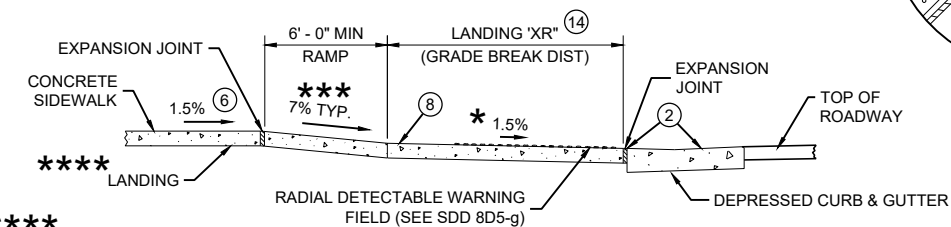
**PLAN VIEW
CURB RAMP TYPE 4B1
(GRADE BREAK DISTANCE GREATER THAN 5 FEET)**

TERRACE STRIP (PLANTING OR NON-TRAVERSABLE SURFACE SHOWN)

* MAXIMUM 2% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK



DETAIL B

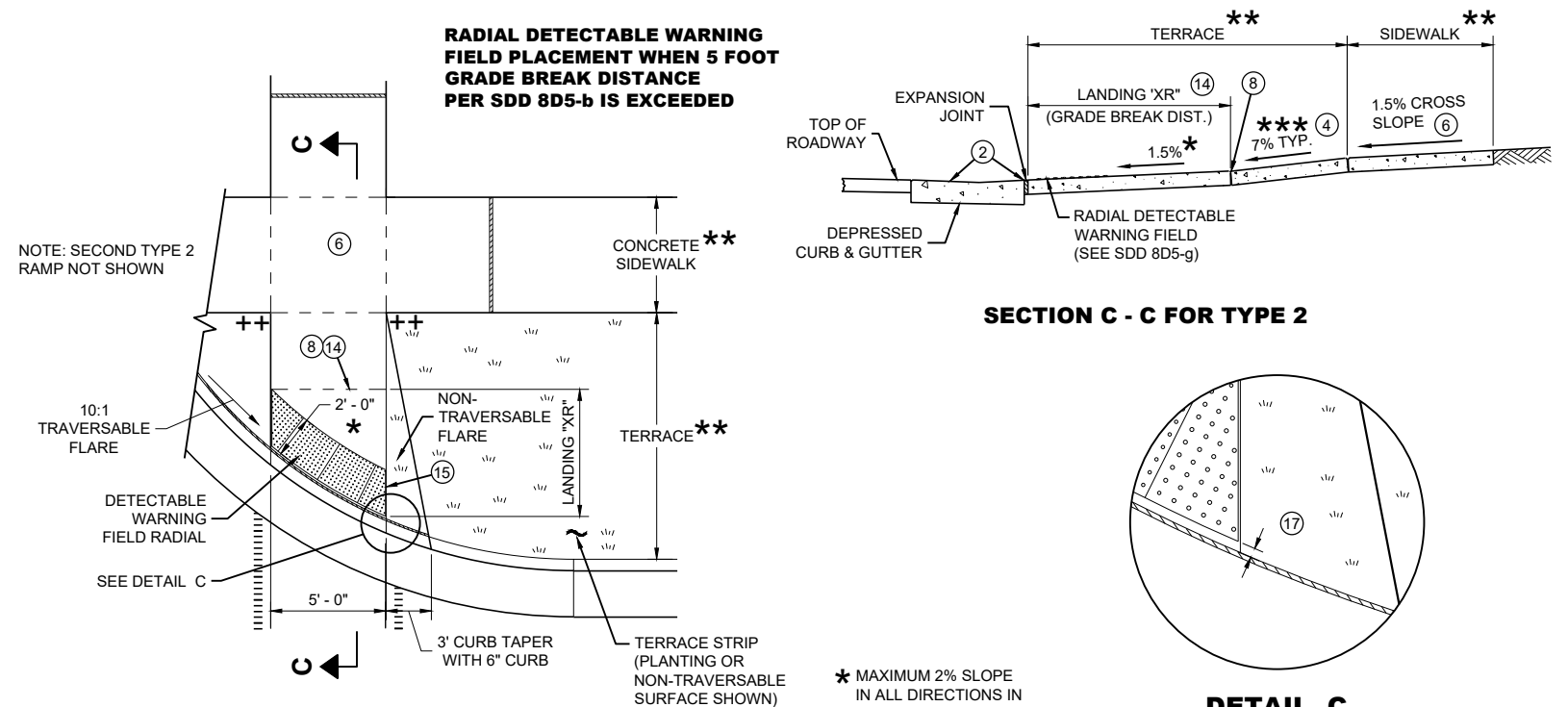


SECTION B - B FOR TYPE 4B1

**** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

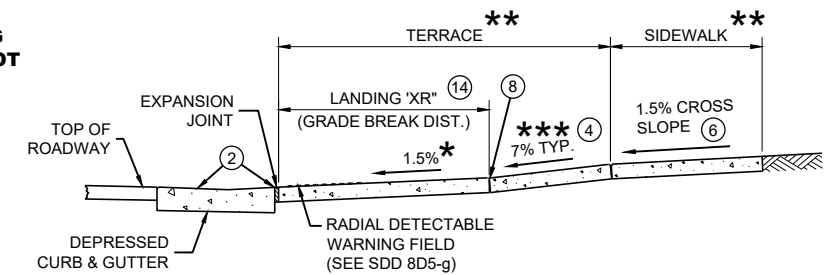
*** MAXIMUM 8.33%

RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-b IS EXCEEDED



**PLAN VIEW
CURB RAMP TYPE 2
(GRADE BREAK DISTANCE GREATER THAN 5 FEET)
(ON LINE WITH SIDEWALK)**

NOTE: SECOND TYPE 2 RAMP NOT SHOWN



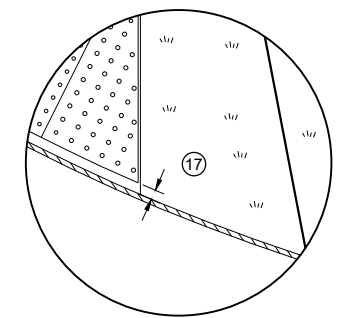
SECTION C - C FOR TYPE 2

* MAXIMUM 2% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK

** WIDTH SHOWN ELSEWHERE IN THE PLANS

*** MAXIMUM 8.33%

++ CONSTRUCT 6" WEDGE TO AVOID CONCRETE BREAKAGE



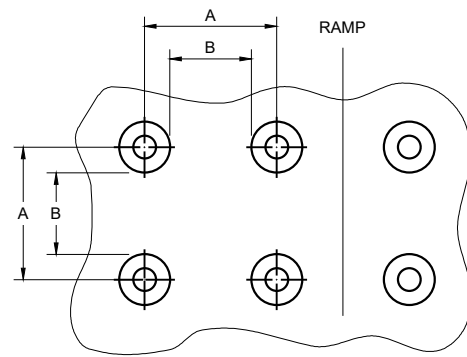
DETAIL C

**CURB RAMPS
RADIAL DETECTABLE WARNING**

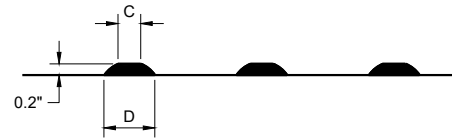
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

	MIN.	MAX.
A	1.6"	2.4"
B	0.65"	1.5"
C	*	*
D	0.9"	1.4"

* THE C DIMENSION IS 50% TO 65% OF THE D DIMENSION.

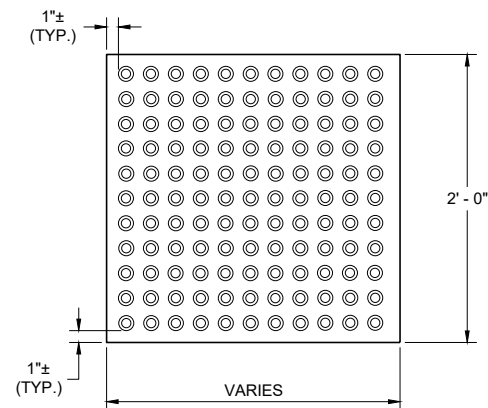


PLAN VIEW

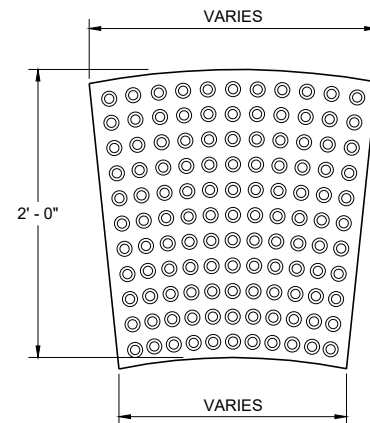


ELEVATION VIEW

**TRUNCATED DOMES
DETECTABLE WARNING PATTERN DETAIL**

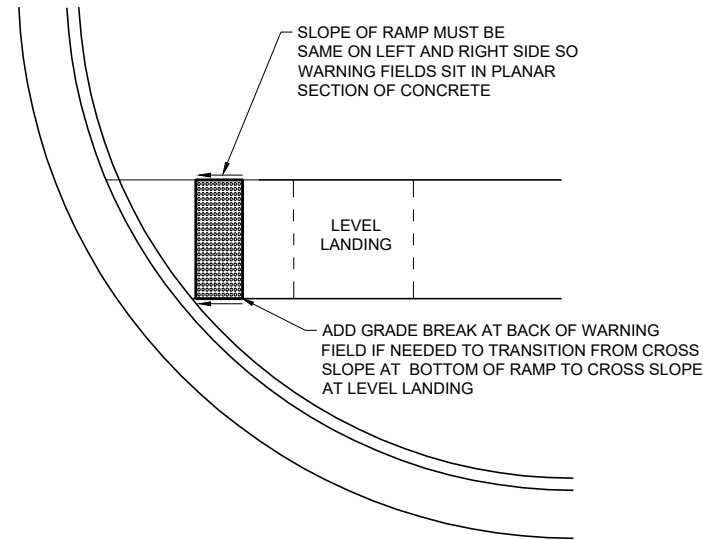


**RECTANGULAR
PLATES**



**RADIAL
PLATES**

**PLAN VIEW
DETECTABLE WARNING FIELDS (TYPICAL)**

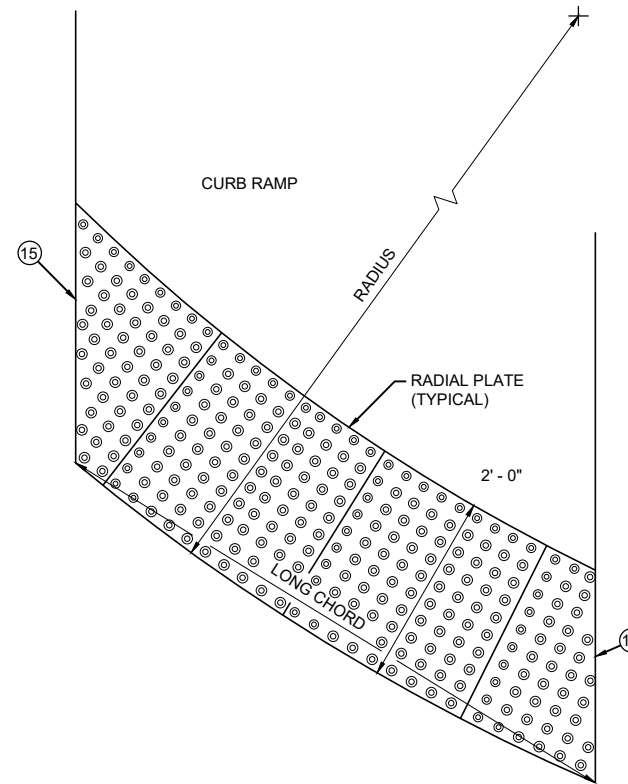


**DETECTABLE WARNING FIELD
PLANAR INSTALLATION**

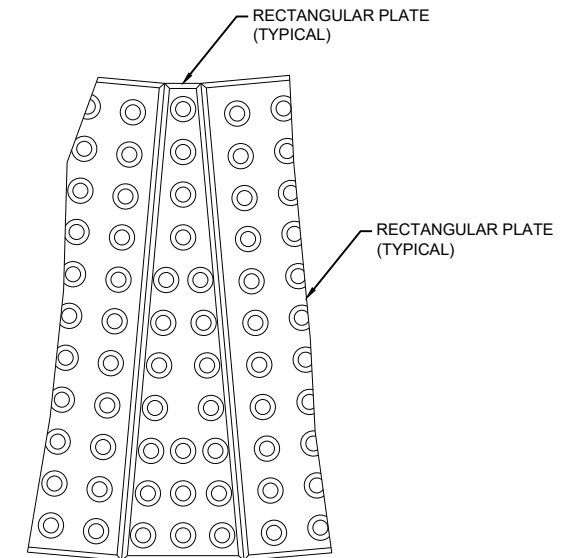
GENERAL NOTES

- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AT A CURB RAMP SHALL BE FROM THE SAME MANUFACTURER.
- PLACE ALL DETECTABLE WARNING FIELD SYSTEMS IN ACCORDANCE TO THE MANUFACTURER'S RECOMMENDATION.
- FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FIELD ARE PROHIBITED.
- DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.
- FOR RADIAL DETECTABLE WARNING FIELD APPLICATIONS WHERE STANDARD RADIAL PLATES ARE NOT AVAILABLE AT AN INTERSECTION CURB RADIUS, A COMBINATION OF SQUARE OR RECTANGULAR PLATES AND RADIAL PLATES MAY BE USED TO FORM RADIAL CONFIGURATION. RADIAL WEDGE PLATES IN COMBINATION WITH SQUARE PLATES ARE ALSO ACCEPTABLE. FOLLOW MANUFACTURER'S RECOMMENDATIONS.
- REFER TO CONTRACT AND STANDARD SPECIFICATIONS FOR FIELD CUTTING REQUIREMENTS.
- DO NOT EMBED IN CONCRETE ANY FIELD-CUT PLATES WITH CUT EDGES SHORTER THAN 6 INCHES. CONSULT WITH MANUFACTURER FOR RE-DRILLING AND ANCHORING REQUIREMENTS OF FIELD-CUT PLATES.

15 FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN 1/8" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.



**PLAN VIEW
RADIAL DETECTABLE
WARNING FIELD ATTRIBUTES**

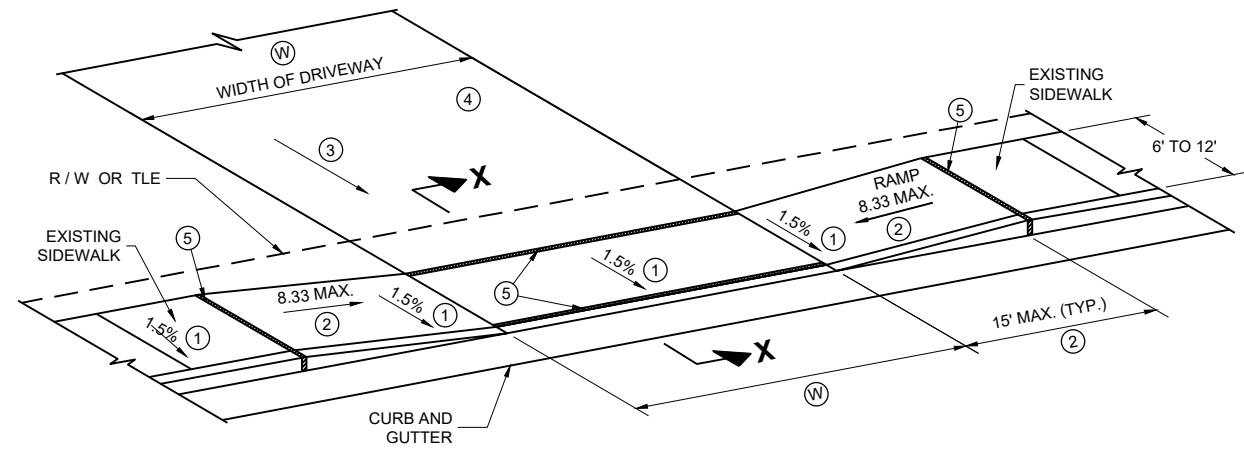


**PLAN VIEW
RADIAL WEDGE PLATE
CONNECTION DETAIL**

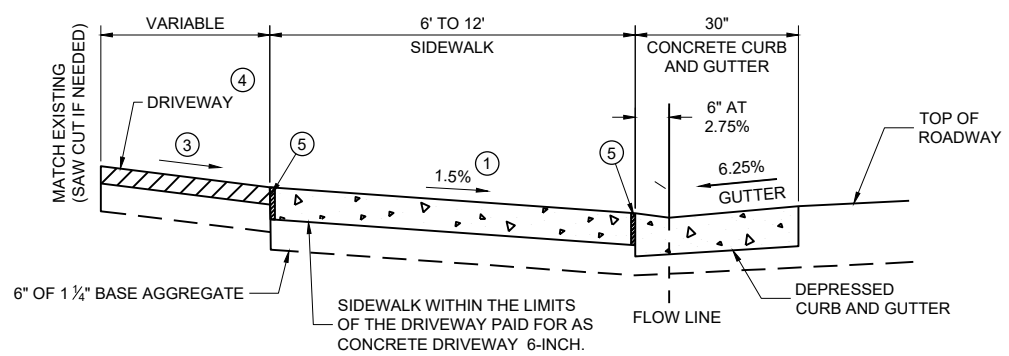
**CURB RAMPS
RECTANGULAR AND RADIAL
DETECTABLE WARNING PLATES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

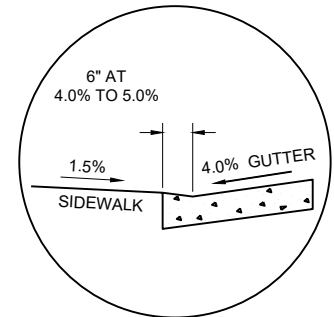
APPROVED
July 2023 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
UNIT SUPERVISOR



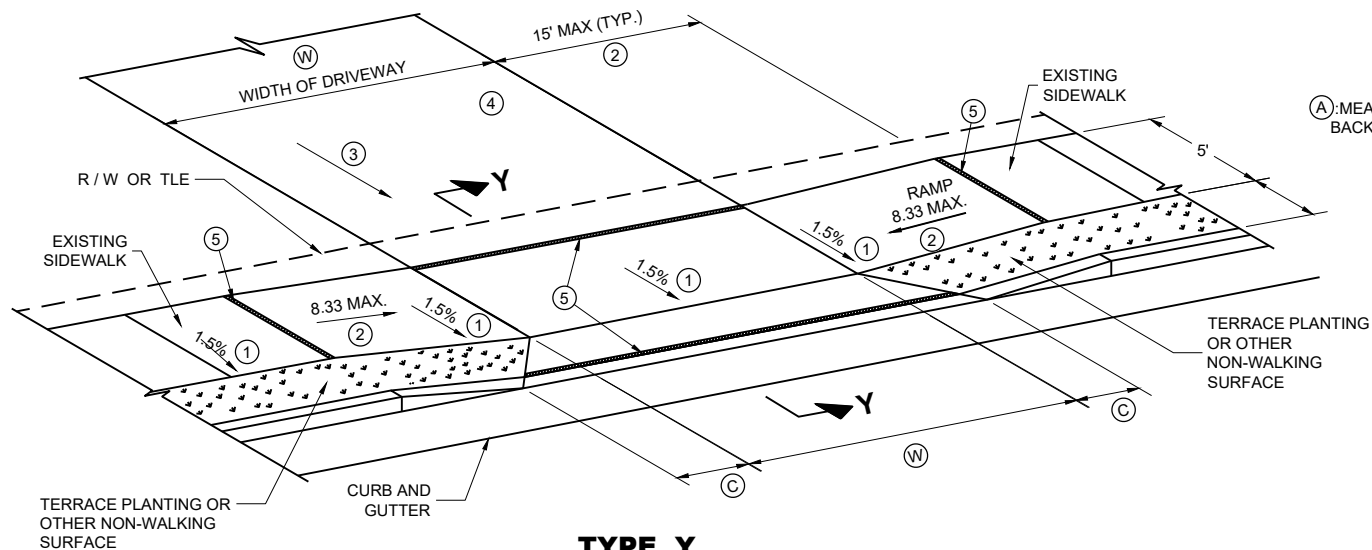
TYPE X
SIDEWALK ABUTS CURB AND GUTTER
TERRACE VARIES 0 TO 3 FEET



SECTION X - X

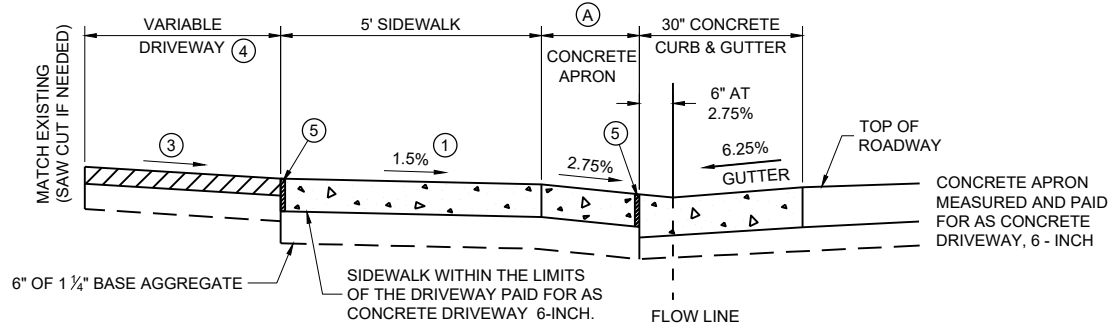


SECTION X - X
4% GUTTER SLOPE



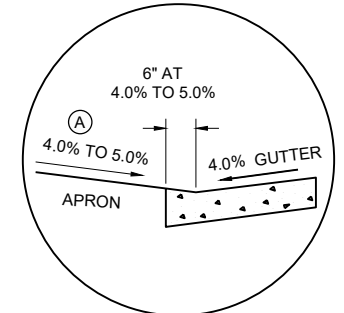
TYPE Y
SIDEWALK WITH NARROWER TERRACE
TERRACE VARIES 4 TO 6 FEET

(A): MEASURE FROM
 BACK OF CURB



NOTE: SIDEWALK MAY BE DEPRESSED IN DRIVEWAY AREAS

SECTION Y - Y
DRIVEWAY DETAIL WITH CONCRETE
CURB AND GUTTER
(URBAN AND SUBURBAN)



SECTION Y - Y
4% GUTTER SLOPE

TABLE Y

(A) FEET	(C) FEET
3.5'	2.0'
4.5'	3.0'
5.5'	3.5'

(W): 12' MIN. - 24' MAX. RESIDENTIAL AND
 NON-COMMERCIAL (PE & FE)
 16' MIN. - 35' MAX. COMMERCIAL (CE)

GENERAL NOTES

PROVIDE CONSTRUCTION JOINTS ALONG THE CENTER OF THE CONCRETE FOR DRIVEWAYS UNDER 20 FEET IN WIDTH AND AT THE THIRD POINTS OVER 20 FEET IN WIDTH.

(W) IS SHOWN ON PLAN AND PROFILE SHEETS.

OFFSETS, ELEVATIONS, AND PERCENT GRADE ARE SHOWN ON THE CROSS SECTIONS.

(1) CONSTRUCTION TOLERANCE OF 0.5%± FOR SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.

(2) THE SIDEWALK RAMP MAXIMUM RUNNING SLOPE SHALL NOT REQUIRE THE RAMP LENGTH TO EXCEED 15 FEET TO AVOID CHASING THE SLOPE INDEFINITELY WHEN CONNECTING TO STEEP GRADES. WHEN APPLYING THE 15 FOOT MAXIMUM LENGTH, THE RUNNING SLOPE OF THE SIDEWALK SHALL BE AS FLAT AS FEASIBLE AND NOT EXCEED THE LONGITUDINAL GRADE OF THE ROADWAY. SLOPE SIDEWALK RAMP TOWARD APRON AS SHOWN WHERE THERE IS NO TERRACE OR WHERE THE TERRACE WIDTH IS LESS THAN 6 FEET WIDE.

(3) **DRIVEWAY SLOPES: DESIRABLE MAXIMUM**
 10.5% UP AWAY FROM SIDEWALK (SAG)
 8.5% DOWN AWAY FROM SIDEWALK (CREST)
 ABSOLUTE MAXIMUM 15% FOR BOTH CREST AND SAG

(4) **DRIVEWAY TYPES**
 · 6-INCH CONCRETE DRIVEWAY PAVEMENT OVER 6-INCH BASE AGGREGATE
 · 2-INCH TO 3-INCH ASPHALTIC SURFACE OVER 6-INCH BASE AGGREGATE
 · 6-INCH BASE AGGREGATE (MAY BE INCREASED FOR CLAY SUBGRADES.)

(5) ½" EXPANSION JOINT FILLER

DRIVEWAY AND
SIDEWALK RAMPS
TYPES X AND Y

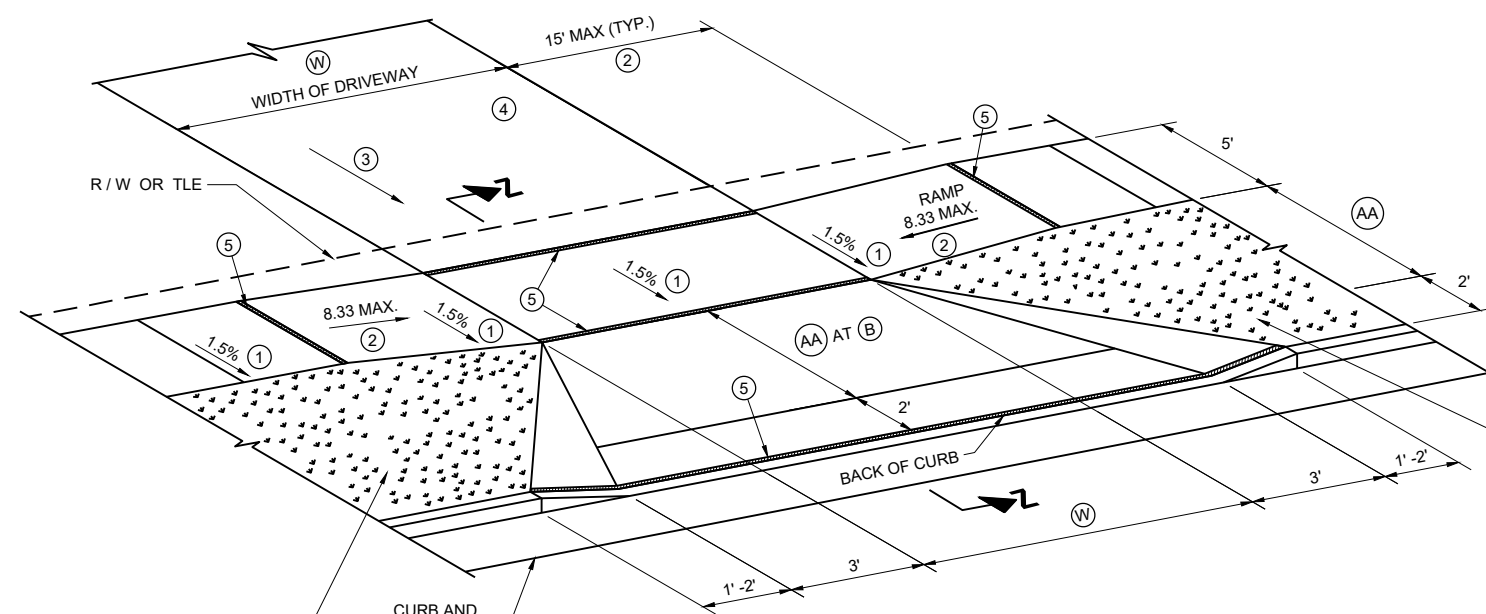
STATE OF WISCONSIN
 DEPARTMENT OF TRANSPORTATION

APPROVED

July 2023
 DATE

/s/ Rodney Taylor
 ROADWAY STANDARDS DEVELOPMENT
 UNIT SUPERVISOR

FHWA



**TYPE Z
SIDEWALK WITH WIDER TERRACE
TERRACE VARIES 7 TO 12 FEET**

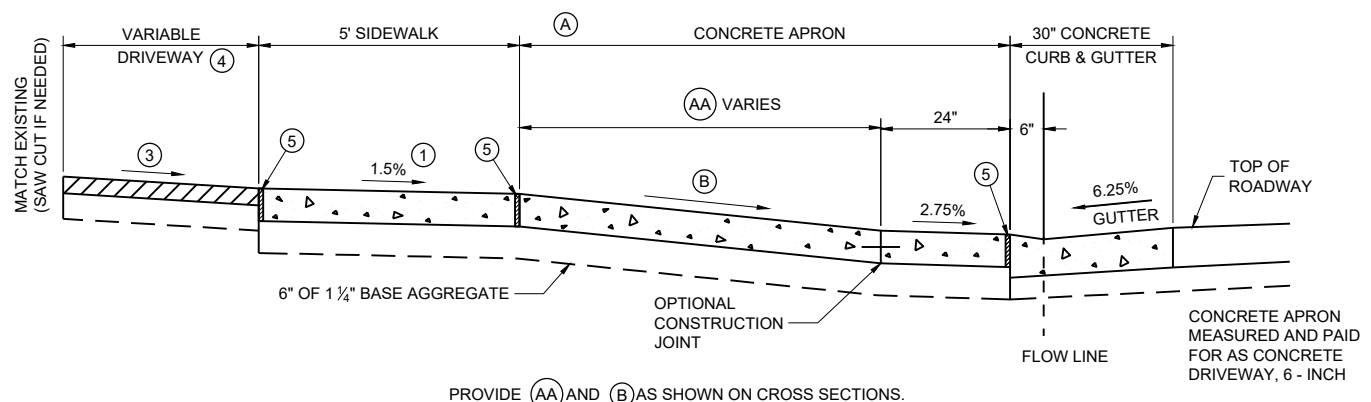
GENERAL NOTES

- PROVIDE CONSTRUCTION JOINTS ALONG THE CENTER OF THE CONCRETE FOR DRIVEWAYS UNDER 20 FEET IN WIDTH AND AT THE THIRD POINTS OVER 20 FEET IN WIDTH.
- (W) IS SHOWN ON PLAN AND PROFILE SHEETS.
- OFFSETS, ELEVATIONS, AND PERCENT GRADE ARE SHOWN ON THE CROSS SECTIONS.
- CONSTRUCTION TOLERANCE OF 0.5%± FOR SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.
 - THE SIDEWALK RAMP MAXIMUM RUNNING SLOPE SHALL NOT REQUIRE THE RAMP LENGTH TO EXCEED 15 FEET TO AVOID CHASING THE SLOPE INDEFINITELY WHEN CONNECTING TO STEEP GRADES. WHEN APPLYING THE 15 FOOT MAXIMUM LENGTH, THE RUNNING SLOPE OF THE SIDEWALK SHALL BE AS FLAT AS FEASIBLE AND NOT EXCEED THE LONGITUDINAL GRADE OF THE ROADWAY.
 - DRIVEWAY SLOPES: DESIRABLE MAXIMUM**
10.5% UP AWAY FROM SIDEWALK (SAG)
8.5% DOWN AWAY FROM SIDEWALK (CREST)
ABSOLUTE MAXIMUM 15% FOR BOTH CREST AND SAG
 - DRIVEWAY TYPES**
- 6-INCH CONCRETE DRIVEWAY PAVEMENT OVER 6-INCH BASE AGGREGATE
- 2-INCH TO 3-INCH ASPHALTIC SURFACE OVER 6-INCH BASE AGGREGATE
- 6-INCH BASE AGGREGATE (MAY BE INCREASED FOR CLAY SUBGRADES.)
 - ½" EXPANSION JOINT FILLER.

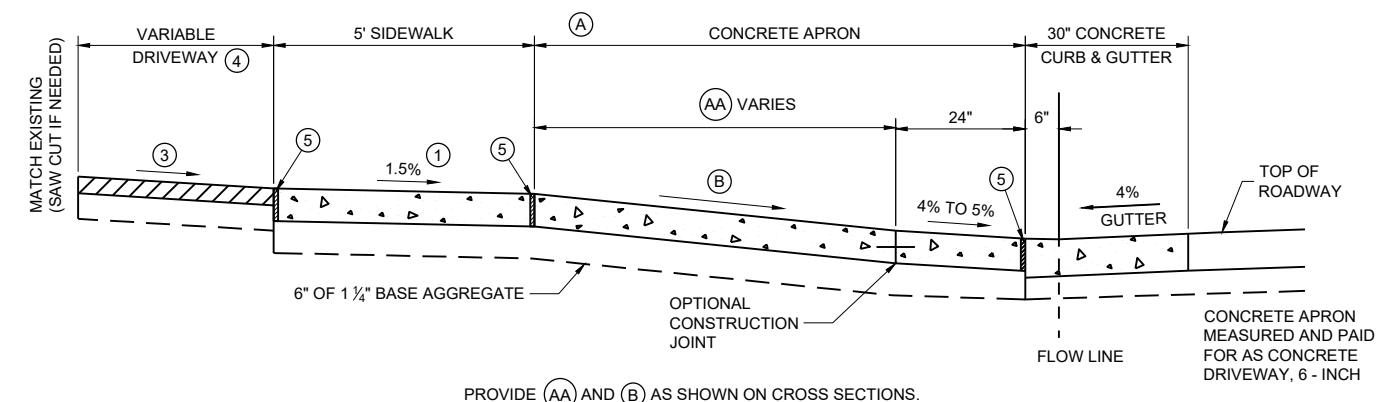
TABLE Z

(AA) FEET	(B) % 6.25% GUTTER	(B) % 4% GUTTER
4.5'	11.5%	9% TO 11.5%
5.5'	9% TO 11.5%	8% TO 11.5%
6.5'	8% TO 11.5%	6% TO 11.5%
7.5'	7% TO 11.5%	6% TO 11.5%
8.5'	6% TO 11.5%	5% TO 11.5%
9.5'	5% TO 11.5%	4% TO 11.5%

(W): 12' MIN. - 24' MAX. RESIDENTIAL AND NON-COMMERCIAL (PE & FE)
16' MIN. - 35' MAX. COMMERCIAL (CE)



6.25% GUTTER SLOPE



4% GUTTER SLOPE

NOTE: SIDEWALK MAY BE DEPRESSED IN DRIVEWAY AREAS FOR (B) VALUES NOT SHOWN IN TABLE Z.
SIDEWALK WITHIN THE LIMITS OF THE DRIVEWAY PAID FOR AS CONCRETE DRIVEWAY 6-INCH.

**SECTION Z - Z
DRIVEWAY DETAIL WITH CONCRETE CURB AND GUTTER
(URBAN AND SUBURBAN)**

**DRIVEWAY AND SIDEWALK RAMPS
TYPE Z**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
July 2023 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
FHWA UNIT SUPERVISOR

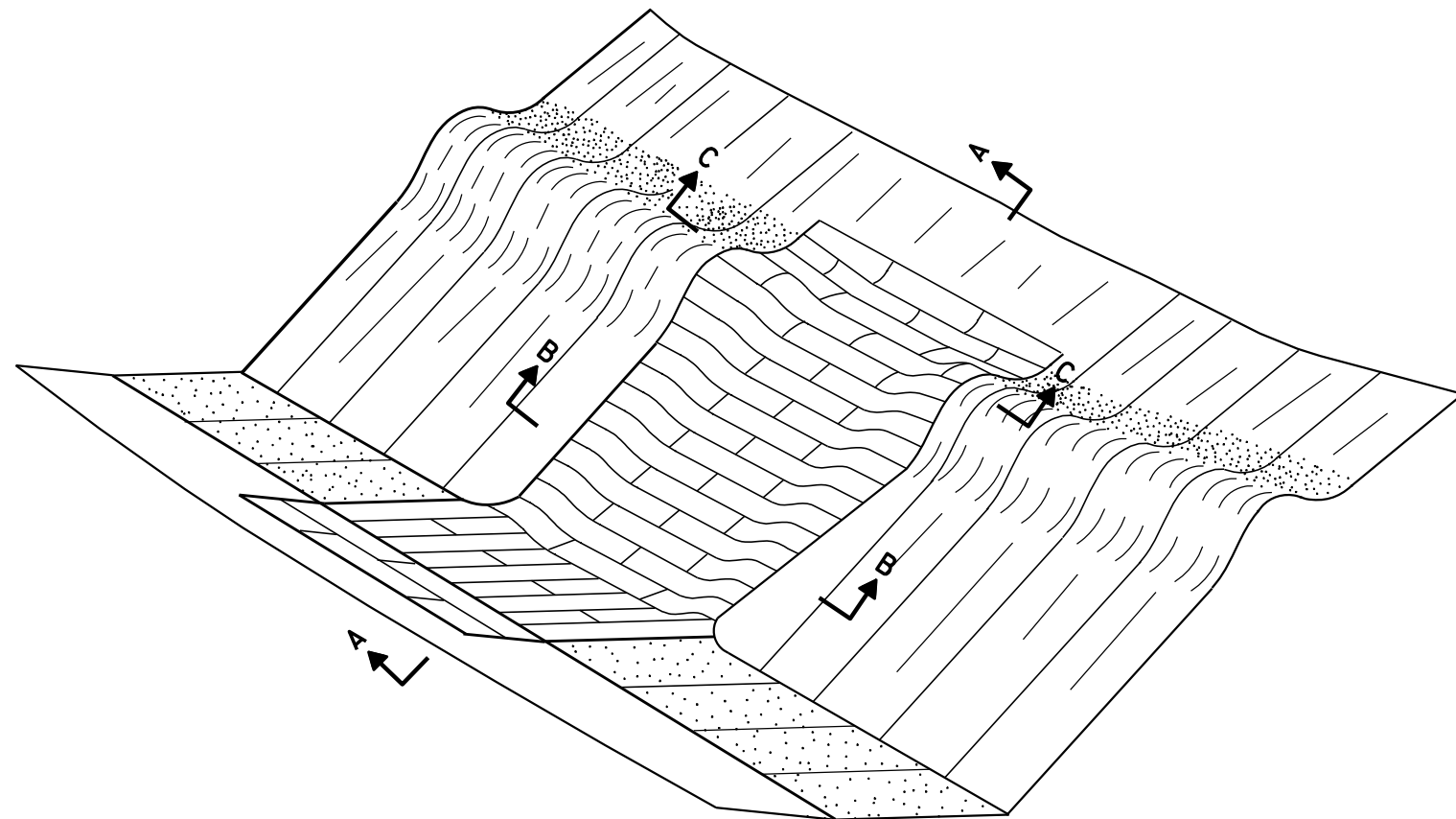
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS.

GRADING AND SHAPING INTERCEPTING EMBANKMENT WILL BE MEASURED AND PAID FOR IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS, SECTION 205, "ROADWAY AND DRAINAGE EXCAVATION".

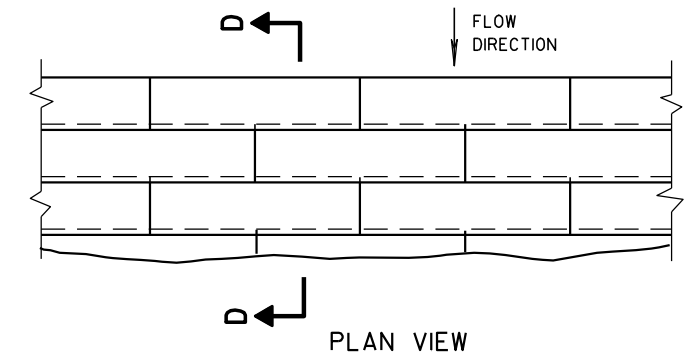
SOD STRIPS SHALL BE LAID TRANSVERSELY TO THE DIRECTION OF THE WATER FLOW.

DIMENSIONS SHALL BE ADJUSTED TO FIT SITE CONDITIONS AS DIRECTED BY THE ENGINEER.



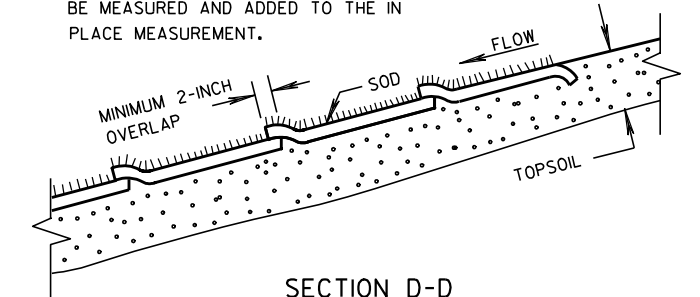
PERSPECTIVE

BACKSLOPE WITH FLUME AND INTERCEPTING EMBANKMENT

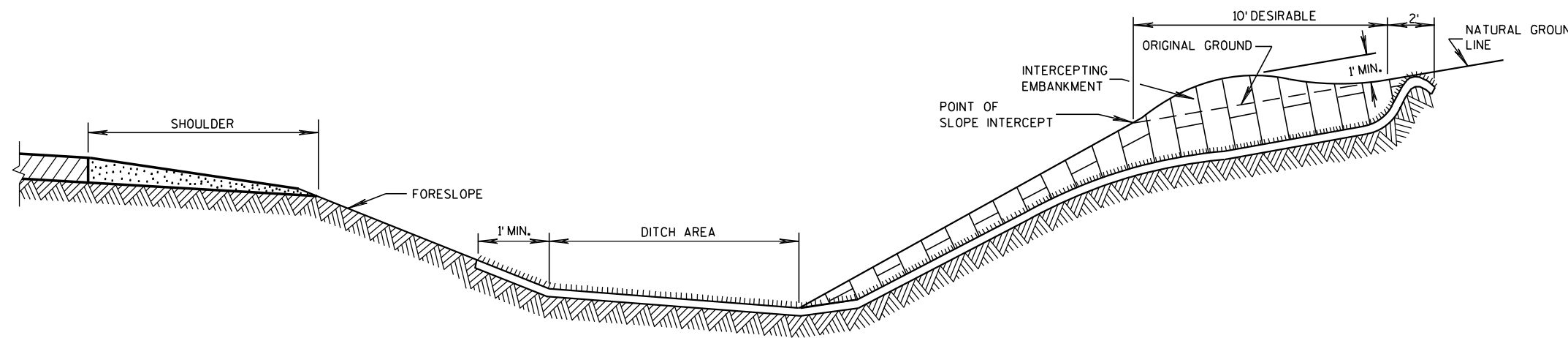


PLAN VIEW

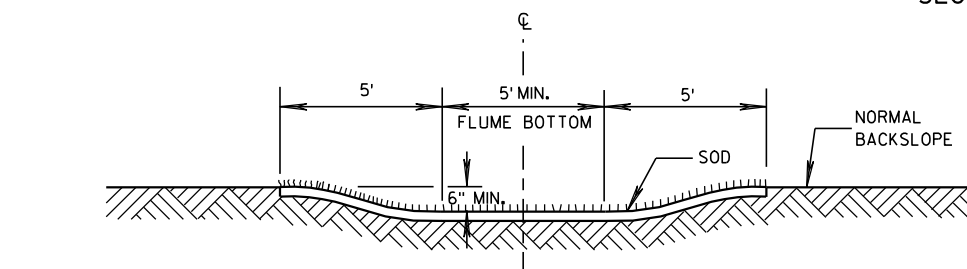
NOTE:
SOD SHALL BE LAID AS SHOWN ON THE PLAN VIEW, AND AS DIRECTED BY THE ENGINEER. THE SOD OVERLAP WILL BE MEASURED AND ADDED TO THE IN PLACE MEASUREMENT.



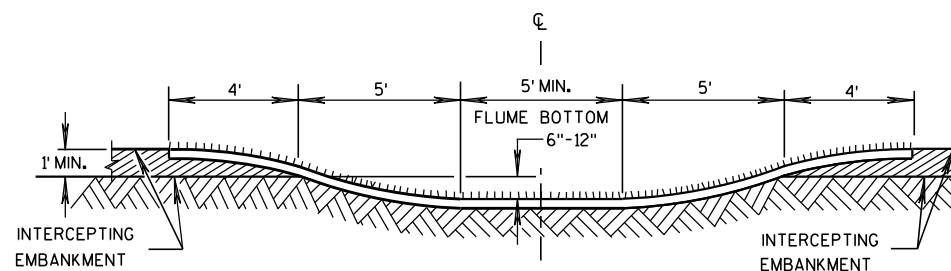
SECTION D-D
DETAIL FOR OVERLAP OF SOD STRIPS



SECTION A-A



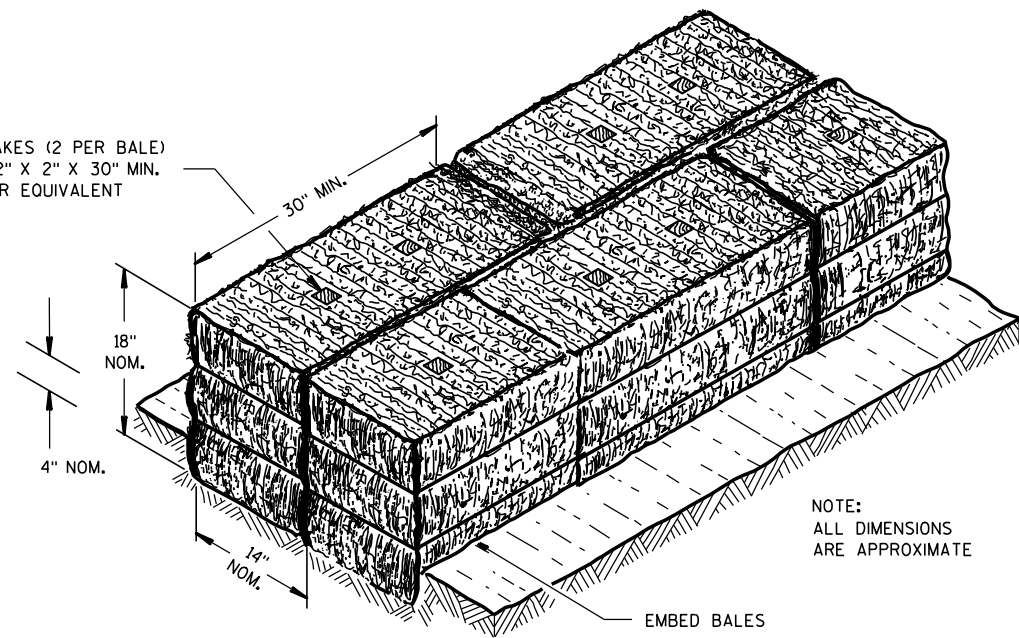
SECTION B-B



SECTION C-C

SODDED BACKSLOPE FLUME AND INTERCEPTING EMBANKMENT	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED	/S/ Rory L. Rhinesmith
10/24/95 DATE	CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA	

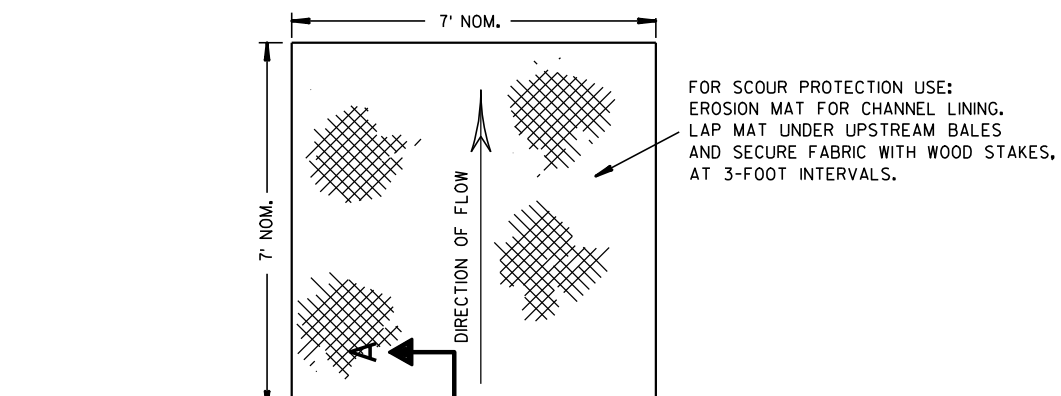
WOOD STAKES (2 PER BALE)
NOMINAL 2" X 2" X 30" MIN.
LENGTH OR EQUIVALENT



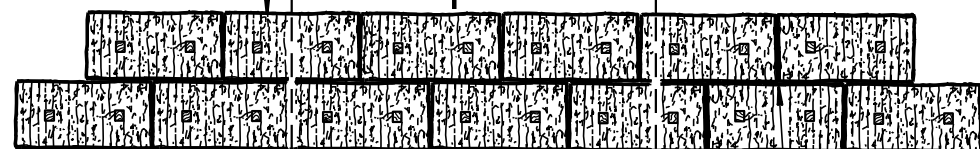
NOTE:
ALL DIMENSIONS
ARE APPROXIMATE

EMBED BALES

SECTION A-A



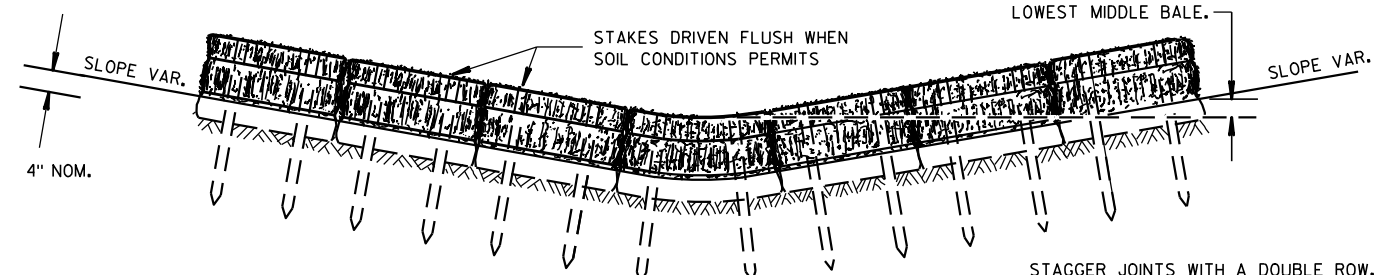
FOR SCOUR PROTECTION USE:
EROSION MAT FOR CHANNEL LINING.
LAP MAT UNDER UPSTREAM BALES
AND SECURE FABRIC WITH WOOD STAKES,
AT 3-FOOT INTERVALS.



STAGGER JOINTS BETWEEN ADJACENT
ROWS OF BALES.

PLAN VIEW

BOTTOM ELEVATION OF END BALE SHALL
BE EQUAL TO OR GREATER THAN TOP OF
LOWEST MIDDLE BALE.



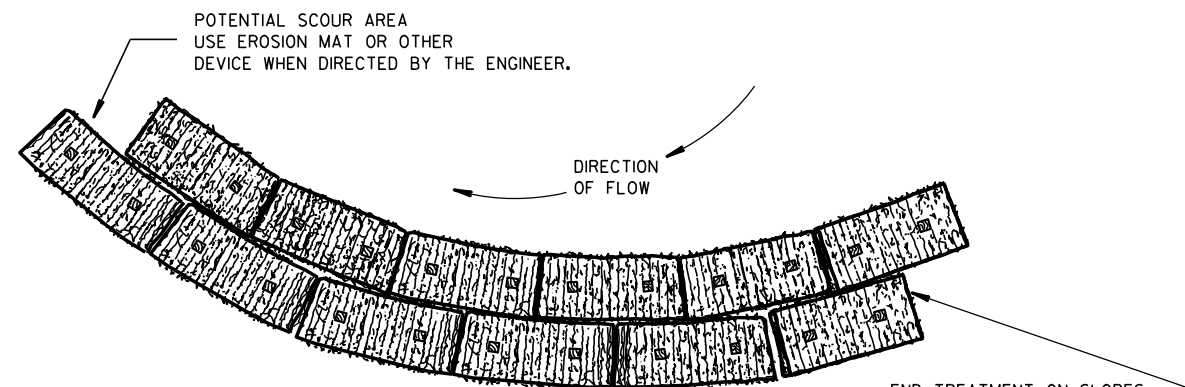
FRONT ELEVATION

TEMPORARY DITCH CHECK USING EROSION BALES ①

GENERAL NOTES

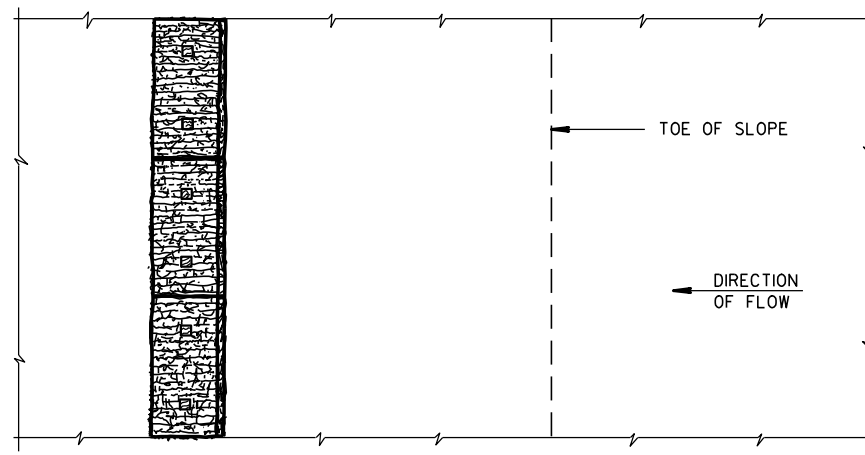
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

- ① TEMPORARY DITCH CHECKS EITHER EROSION BALES OR MANUFACTURED SHALL BE PAID FOR UNDER THE BID ITEM OF TEMPORARY DITCH CHECK. THE DEPARTMENT WILL NOT PAY FOR TEMPORARY DITCH CHECKS CONSTRUCTED OF A SINGLE ROW OF EROSION BALES.

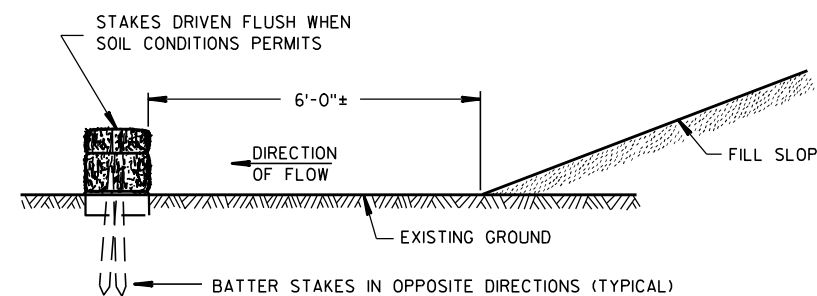


PLAN VIEW

WHEN ALTERING THE DIRECTION OF FLOW



PLAN VIEW



FRONT ELEVATION

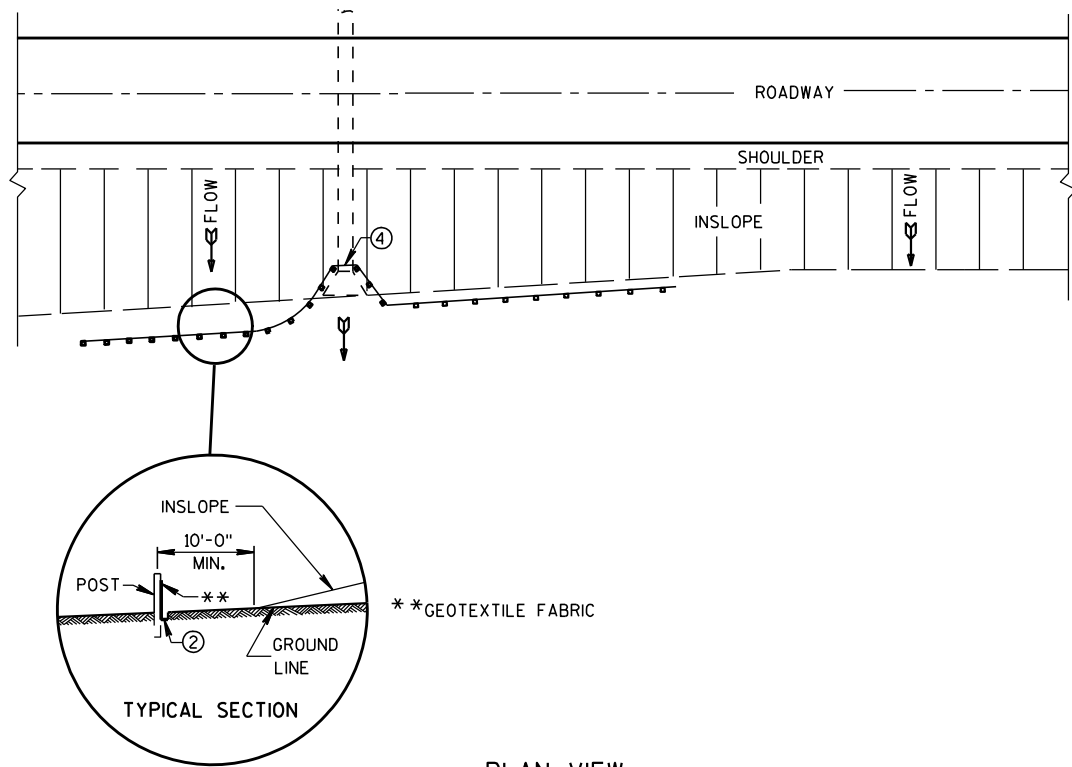
WHEN EXISTING GROUND SLOPES AWAY FROM FILL SLOPE

EROSION BALES FOR SHEET FLOW

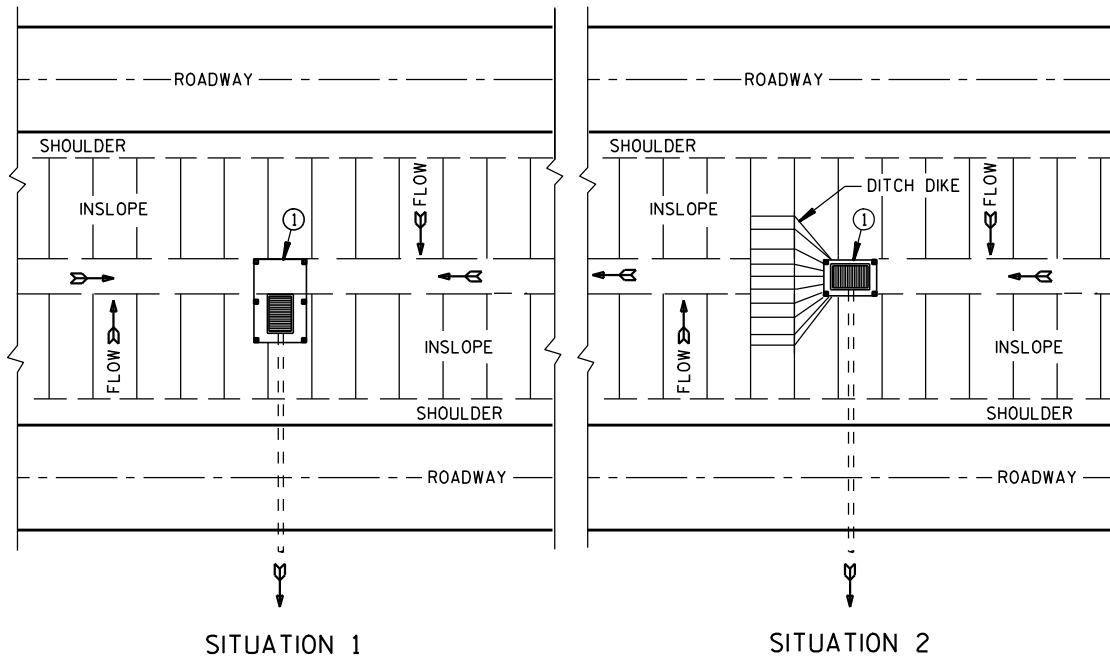
TYPICAL INSTALLATIONS OF
EROSION BALES / TEMPORARY
DITCH CHECKS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
 6/04/02 /S/ Beth Canestra
 DATE CHIEF ROADWAY DEVELOPMENT ENGINEER
 FHWA



PLAN VIEW
TYPICAL APPLICATION OF SILT FENCE

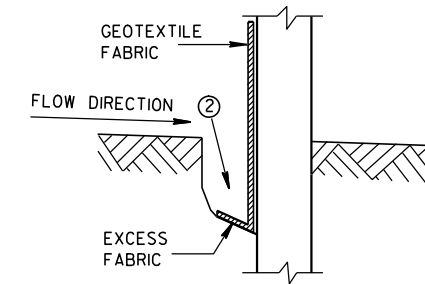


SITUATION 1 SITUATION 2
PLAN VIEW
SILT FENCE AT MEDIAN SURFACE DRAINS

GENERAL NOTES

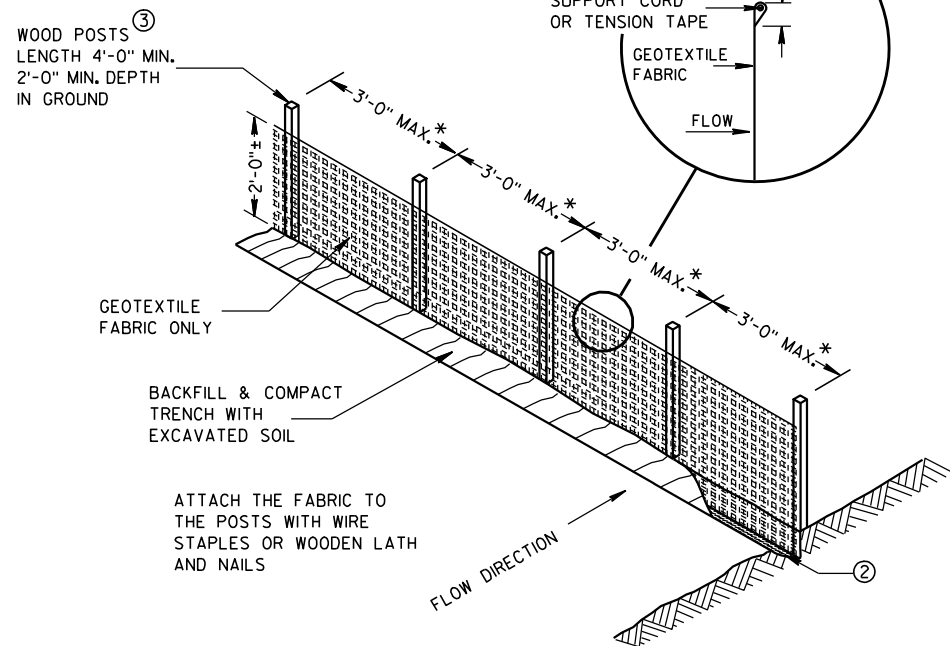
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

- ① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- ③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1 1/8" X 1 1/8" OF OAK OR HICKORY.
- ④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- ⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



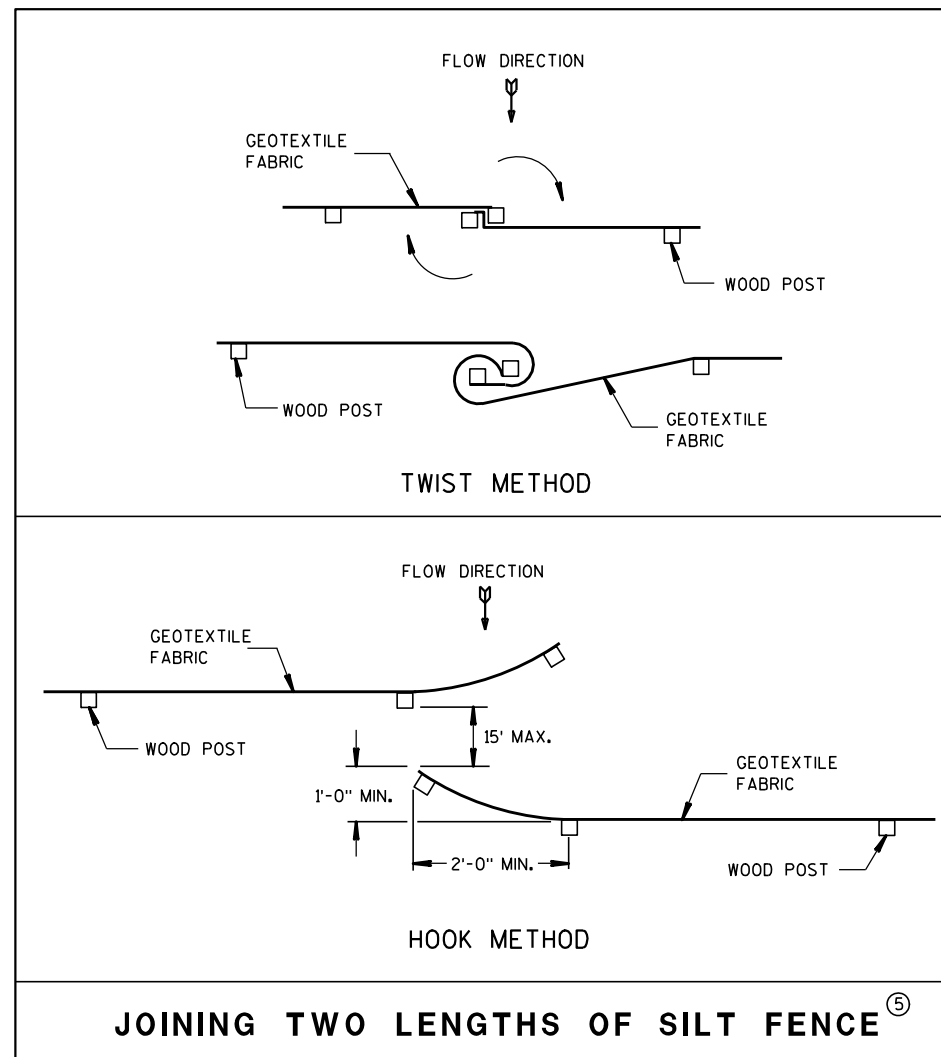
TRENCH DETAIL

NOTE: ADDITIONAL POST DEPTH OR TIE BACKS MAY BE REQUIRED IN UNSTABLE SOILS

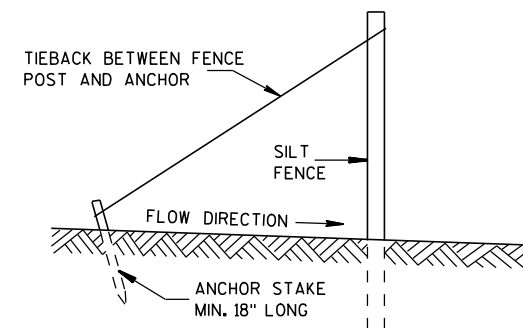


SILT FENCE

* NOTE: 8'-0" POST SPACING ALLOWED IF A WOVEN GEOTEXTILE FABRIC IS USED.



JOINING TWO LENGTHS OF SILT FENCE ⑤

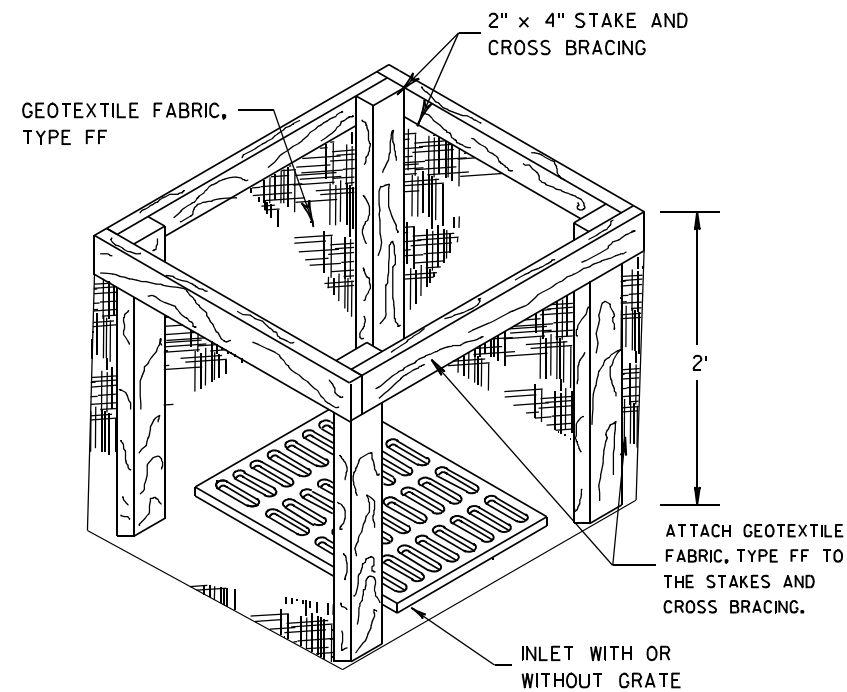
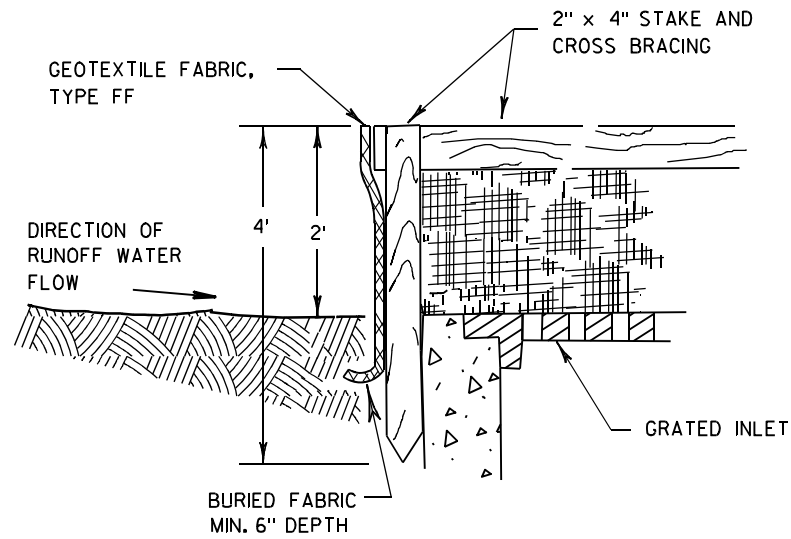


SILT FENCE TIE BACK
(WHEN REQUIRED BY THE ENGINEER)

SILT FENCE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
4-29-05 /S/ Beth Canestra
DATE CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA



INLET PROTECTION, TYPE A

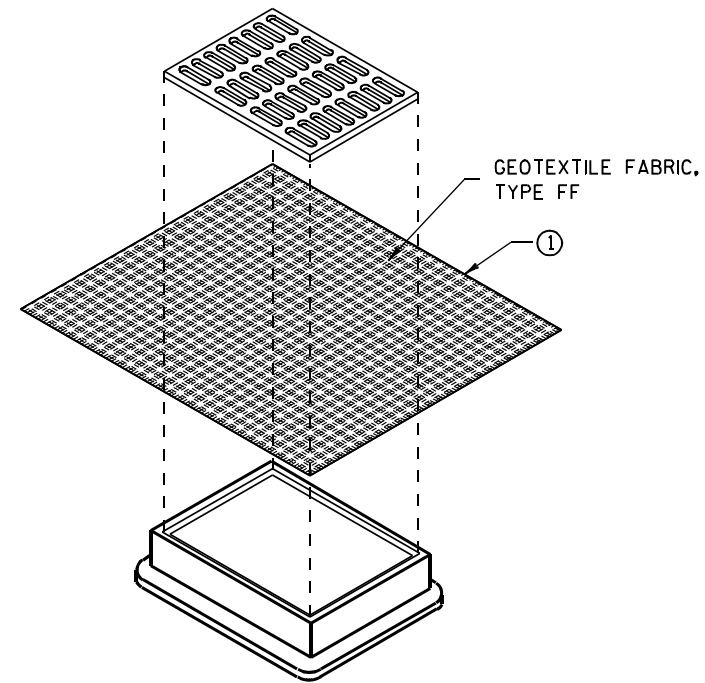
GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

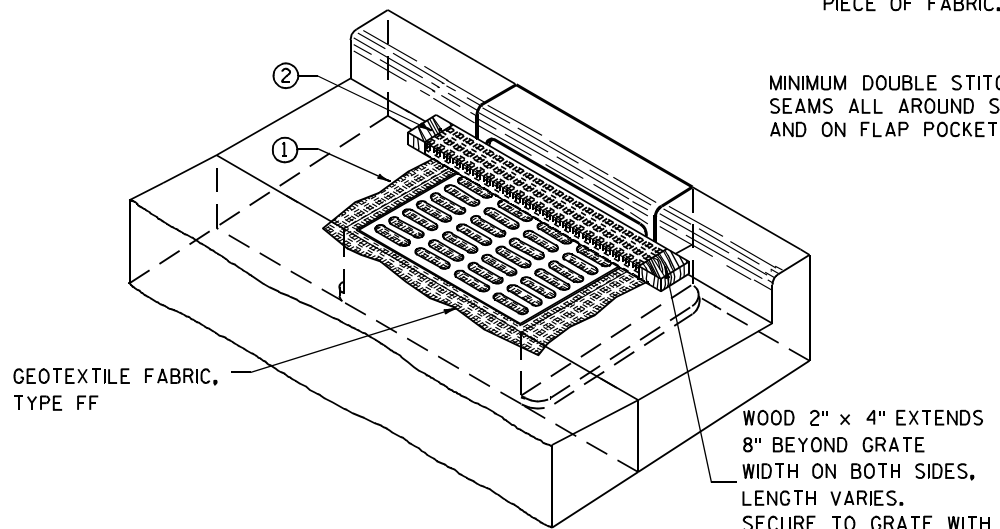
WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- ① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- ② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- ③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



**INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)**

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE C (WITH CURB BOX)

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

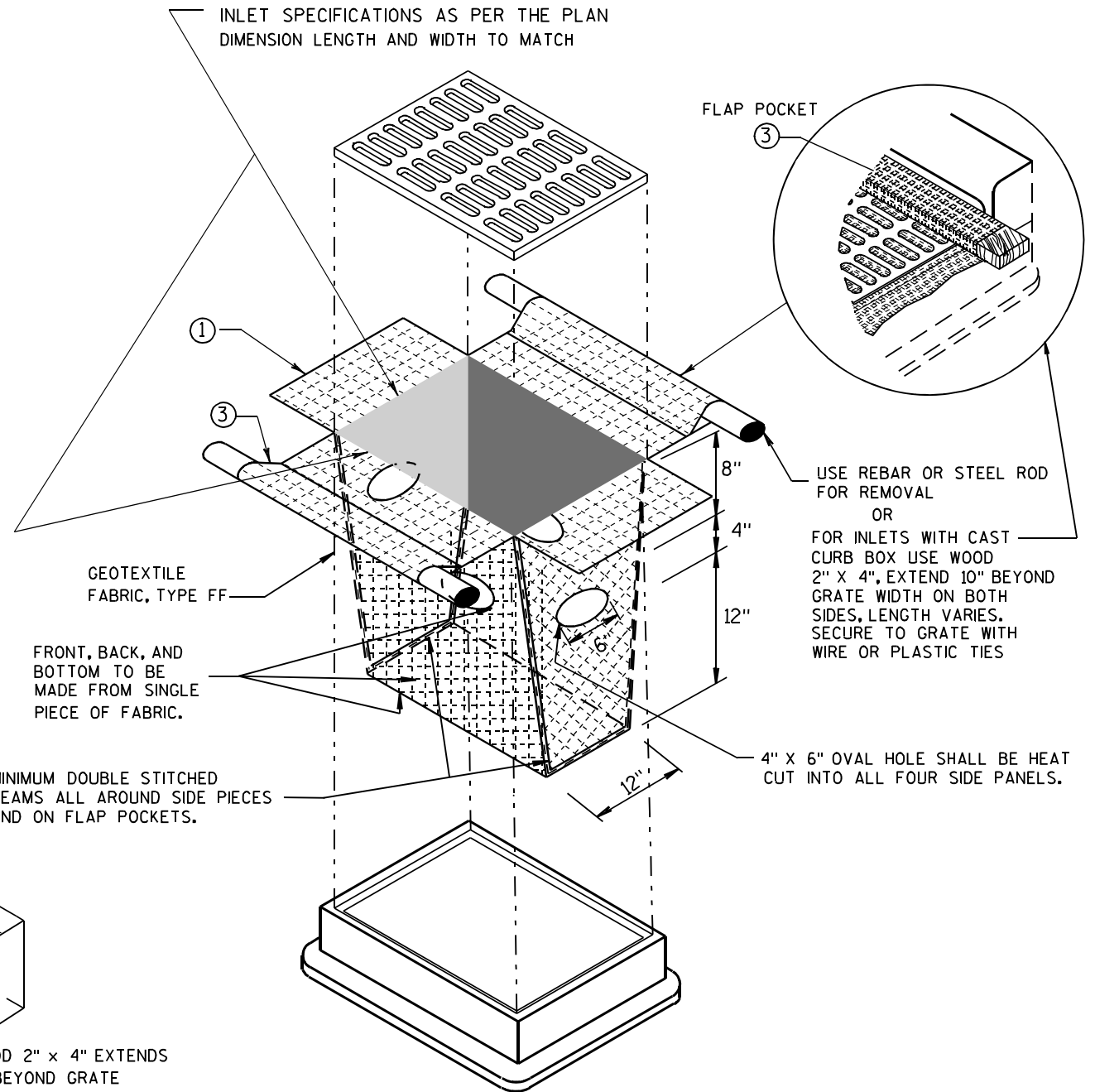
THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.



INLET PROTECTION, TYPE D

(CAN BE INSTALLED IN ANY INLET TYPE WITH OR WITHOUT A CURB BOX AS PER NOTE ②)

INLET PROTECTION TYPE A, B, C, AND D	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 10/16/02 DATE	/s/ Beth Connestra CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA	

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

TRACKING PAD SHALL BE INSPECTED DAILY. DEFICIENT AREAS SHALL BE REPAIRED OR REPLACED IMMEDIATELY.

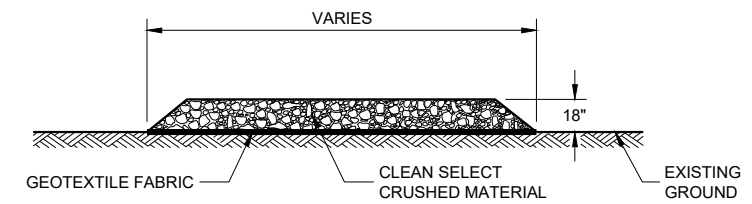
TRACKING PAD TO BE REMOVED AFTER CONSTRUCTION IS COMPLETED.

TRACKING PAD SHALL BE THE FULL WIDTH OF THE EGRESS POINT.

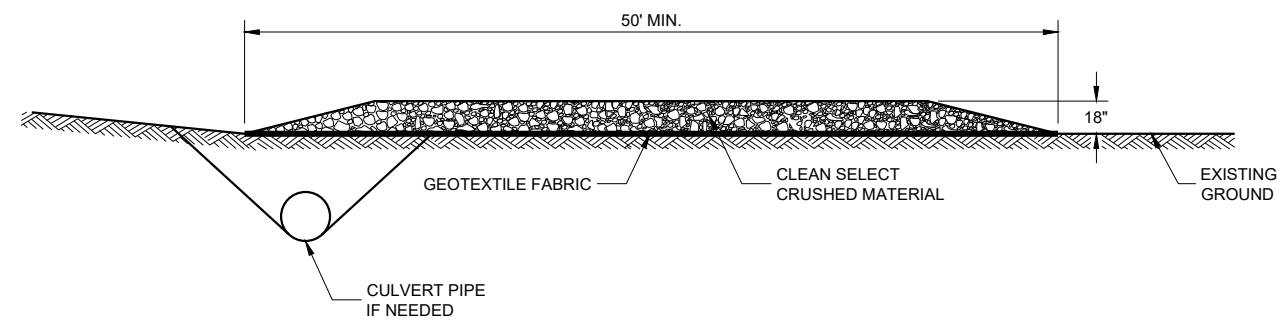
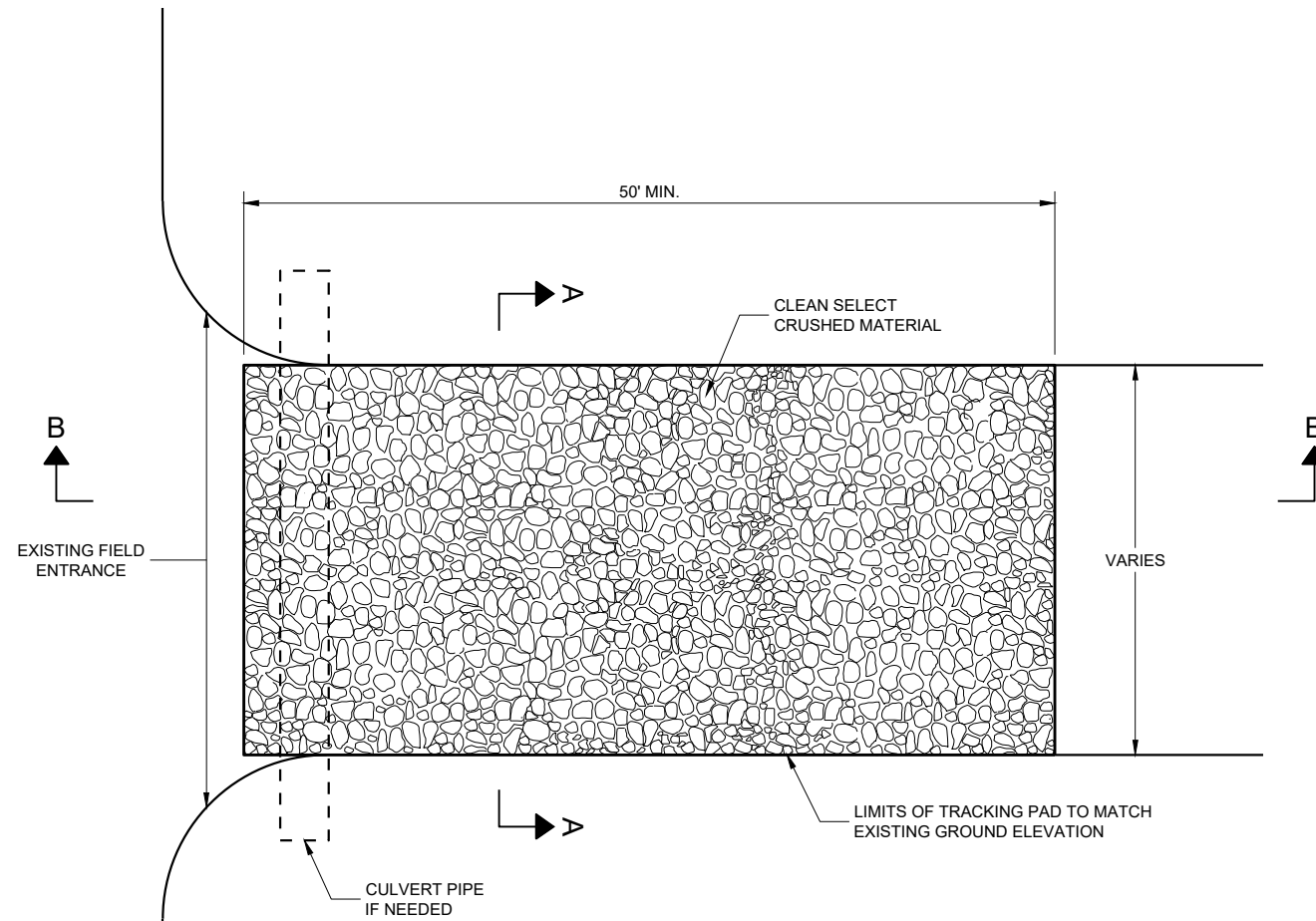
SURFACE WATER MUST BE PREVENTED FROM PASSING THROUGH THE TRACKING PAD. FLOWS SHALL BE DIVERTED AWAY, AROUND OR CONVEYED UNDER THE TRACKING PAD.

CULVERT PIPE OR OTHER BMP USED TO DIVERT WATER AWAY, AROUND OR UNDER THE TRACKING PAD SHALL BE DESIGNED TO CONVEY THE 2 YEAR - 24 HOUR EVENT.

THE COST OF ADDITIONAL BMP TO DIVERT WATER ARE INCIDENTAL TO THE TRACKING PAD BID ITEM.



SECTION A - A



SECTION B - B

TRACKING PAD

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
3/24/2011 DATE /S/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT ENGINEER

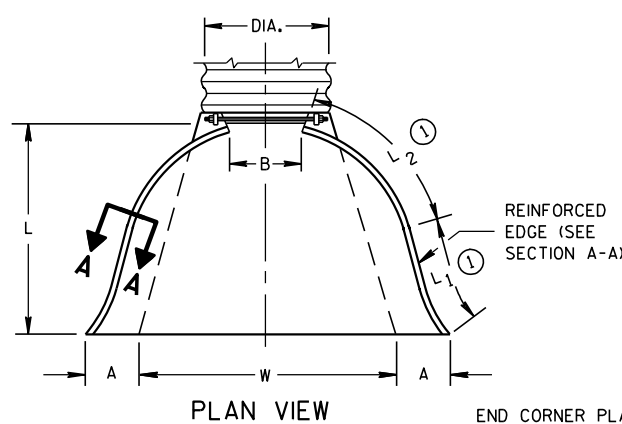
FHWA

METAL APRON ENDWALLS											
PIPE DIA. (IN.)	MIN. THICK. (Inches)		DIMENSIONS (Inches)							APPROX. SLOPE	BODY
	STEEL	ALUM.	A (±1")	B (MAX.)	H (±1")	L (±1 1/2")	L1	L2	W (±2")		
12	.064	.060	6	6	6	21	12	17 1/2	24	2 1/2 to 1	1 Pc.
15	.064	.060	7	8	6	26	14	21 3/4	30	2 1/2 to 1	1 Pc.
18	.064	.060	8	10	6	31	15	28 1/4	36	2 1/2 to 1	1 Pc.
21	.064	.060	9	12	6	36	18	29 5/8	42	2 1/2 to 1	1 Pc.
24	.064	.075	10	13	6	41	18	37 1/4	48	2 1/2 to 1	1 Pc.
30	.079	.075	12	16	8	51	18	52 1/4	60	2 1/2 to 1	1 Pc.
36	.079	.105	14	19	9	60	24	59 3/4	72	2 1/2 to 1	2 Pc.
42	.109	.105	16	22	11	69	24	75 5/8	84	2 1/2 to 1	2 Pc.
48	.109	.105	18	27	12	78	24	81	90	2 1/4 to 1	3 Pc.
54	.109	.105	18	30	12	84	30	85 1/2	102	2 1/4 to 1	3 Pc.
60	.109x	.105x	18	33	12	87	—	—	114	2 to 1	3 Pc.
66	.109x	.105x	18	36	12	87	—	—	120	2 to 1	3 Pc.
72	.109x	.105x	18	39	12	87	—	—	126	2 to 1	3 Pc.
78	.109x	.105x	18	42	12	87	—	—	132	1 1/2 to 1	3 Pc.
84	.109x	.105x	18	45	12	87	—	—	138	1 1/2 to 1	3 Pc.
90	.109x	.105x	18	37	12	87	—	—	144	1 1/2 to 1	3 Pc.
96	.109x	.105x	18	35	12	87	—	—	150	1 1/2 to 1	3 Pc.

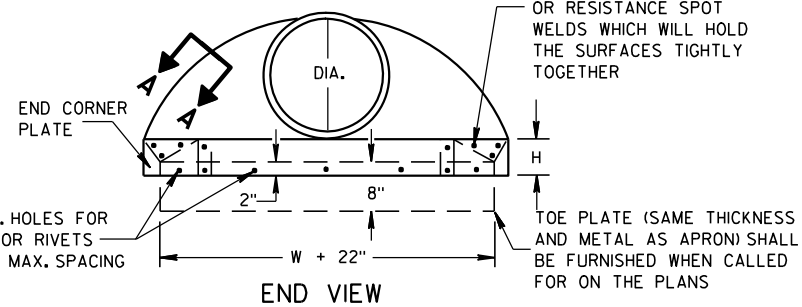
* EXCEPT CENTER PANEL SEE GENERAL NOTES

REINFORCED CONCRETE APRON ENDWALLS									
PIPE DIA. (IN.)	DIMENSIONS (Inches)							APPROX. SLOPE	
	T	A	B	C	D	E	G		
12	2	4	24	48 1/8	72 1/8	24	2	3 to 1	
15	2 1/4	6	27	46	73	30	2 1/4	3 to 1	
18	2 1/2	9	27	46	73	36	2 1/2	3 to 1	
21	2 3/4	9	36	37 1/2	73 1/2	42	2 3/4	3 to 1	
24	3	9 1/2	43 1/2	30	73 1/2	48	3	3 to 1	
27	3 1/4	10 1/2	49 1/2	24	73 1/2	54	3 1/4	3 to 1	
30	3 1/2	12	54	19 3/4	73 1/2	60	3 1/2	3 to 1	
36	4	15	63	34 3/4	97 3/4	72	4	3 to 1	
42	4 1/2	21	63	35	98	78	4 1/2	3 to 1	
48	5	24	72	26	98	84	5	3 to 1	
54	5 1/2	27	65	33 1/4-35	98 1/4-100	90	5 1/2	2 1/2 to 1	
60	6	30-35	60	39	99	96	5	2 to 1	
66	6 1/2	24-30	72-78	21-27	99	102	5 1/2	2 to 1	
72	7	24-36	78	21	99	108	6	2 to 1	
78	7 1/2	24-36	78	21	99	114	6 1/2	2 to 1	
84	8	36	90 1/2	21	111 1/2	120	6 1/2	1 1/2 to 1	
90	8 1/2	41	87 1/2	24	111 1/2	132	6 1/2	1 1/2 to 1	

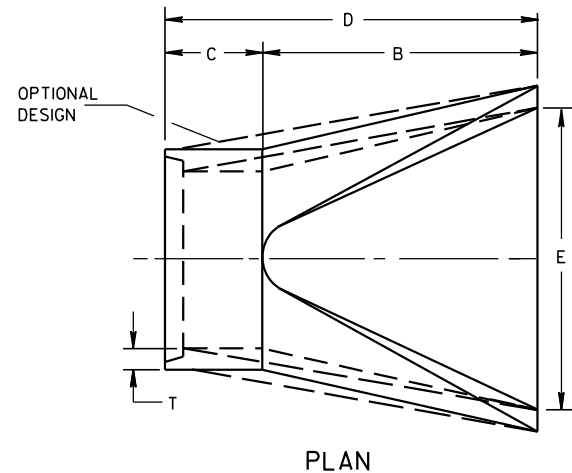
* MINIMUM
** MAXIMUM



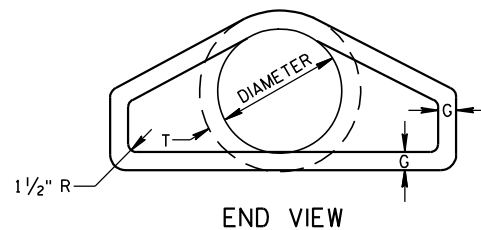
END CORNER PLATES MAY BE FASTENED TO APRON PROPER BY BOLTS, RIVETS, OR RESISTANCE SPOT WELDS WHICH WILL HOLD THE SURFACES TIGHTLY TOGETHER



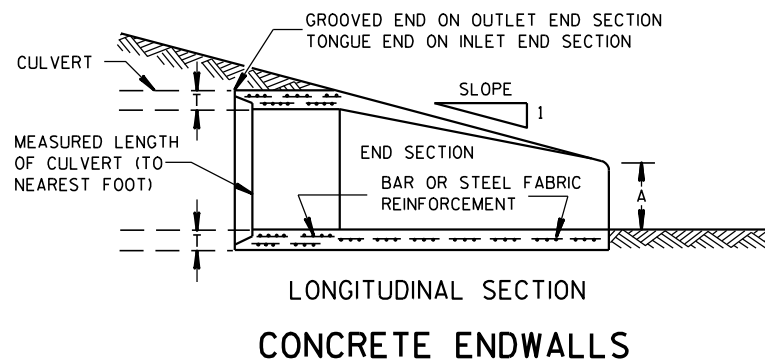
SIDE ELEVATION
METAL ENDWALLS



PLAN

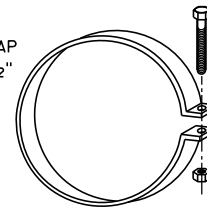


END VIEW

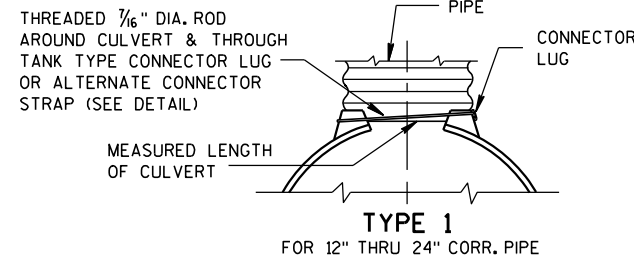


LONGITUDINAL SECTION
CONCRETE ENDWALLS

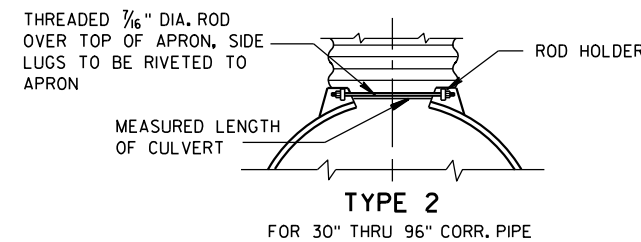
1" WIDE, 12 GA. (0.109" THICK) GALVANIZED STRAP WITH STANDARD 6" X 1/2" BAND BOLT AND NUT



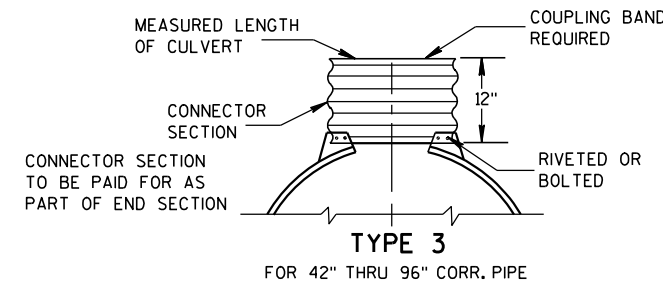
ALTERNATE FOR TYPE 1 CONNECTION
END SECTION CONNECTOR STRAP



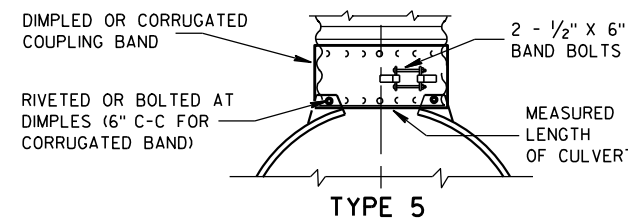
TYPE 1
FOR 12" THRU 24" CORR. PIPE



TYPE 2
FOR 30" THRU 96" CORR. PIPE



TYPE 3
FOR 42" THRU 96" CORR. PIPE



ALTERNATE FOR:
ALL SIZES CORRUGATED CIRCULAR PIPE

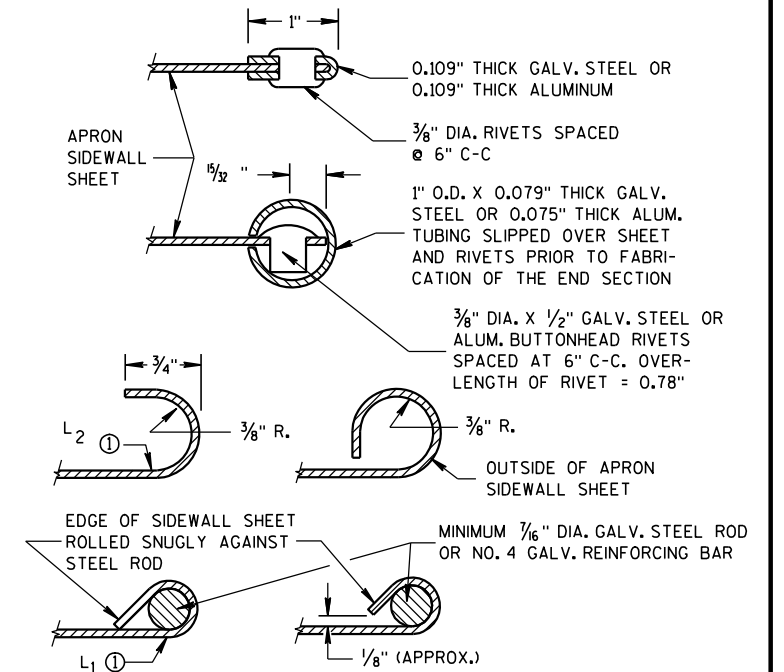
NOTE: DIMPLED BAND FITS OVER OUTSIDE OF ENDWALL, AND CORRUGATED BAND FITS INSIDE ENDWALL. DIMPLED BAND MAY BE USED WITH HELICALLY CORRUGATED PIPE.

FOR CIRCUMFERENTIALLY CORRUGATED PIPE USE ENDWALL CONNECTION DETAILS 1, 2, 3 OR 5 AS APPLICABLE.

FOR HELICALLY CORRUGATED PIPE USE ENDWALL CONNECTION DETAILS 1, 2 OR 5.

FOR HELICALLY CORRUGATED PIPES WITH TWO CIRCUMFERENTIAL CORRUGATIONS AT EACH END USE ENDWALL CONNECTION DETAILS 1, 2 OR 3.

CONNECTION DETAILS



SECTION A-A

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

CONCRETE CULVERT ENDWALLS MAY NOT BE USED WITH GALVANIZED STEEL OR ALUMINUM CULVERT PIPE OR VICE VERSA. GALVANIZED STEEL OR ALUMINUM ENDWALLS SHALL NORMALLY BE INSTALLED ON CULVERT PIPE OF THE SAME METAL.

ALL THREE PIECE STEEL APRON ENDWALLS FOR 60" DIAMETER PIPE AND LARGER SHALL HAVE 0.109" SIDES AND 0.138" CENTER PANELS. ALL THREE PIECE ALUMINUM APRON ENDWALLS FOR 60" DIAMETER PIPE AND LARGER SHALL HAVE 0.105" SIDES AND 0.134" CENTER PANELS. THE WIDTH OF CENTER PANELS SHALL BE GREATER THAN 20 PERCENT OF THE PIPE PERIMETER.

LAP SEAMS SHALL BE TIGHTLY JOINED BY GALVANIZED RIVETS OR BOLTS FOR STEEL UNITS AND ALUMINUM RIVETS AND BOLTS FOR ALUMINUM UNITS. FOR THE 60" THROUGH 96" DIAMETER APRON ENDWALL SIZES, THE REINFORCED EDGES AND CENTER PANEL SEAMS SHALL BE FURTHER REINFORCED WITH GALVANIZED STEEL OR ALUMINUM STIFFENER ANGLES. THE ANGLES SHALL BE ATTACHED BY GALVANIZED NUTS AND BOLTS FOR STEEL UNITS AND ALUMINUM NUTS AND BOLTS FOR ALUMINUM UNITS.

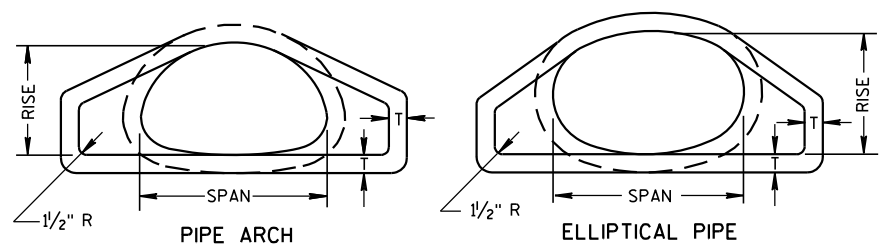
WHERE TWO OR MORE PIPES WITH APRON ENDWALLS ARE LAID ADJACENT TO EACH OTHER, THEY SHALL BE SEPARATED BY A DISTANCE SUFFICIENT TO PROVIDE A MINIMUM CLEARANCE OF 6 INCHES BETWEEN APRON ENDWALLS.

① FOR PIPE SIZES UP TO 60" DIAMETER, A 180° ROLLED EDGE MAY BE USED INSTEAD OF STEEL ROD REINFORCEMENT. SEE SECTION A-A.

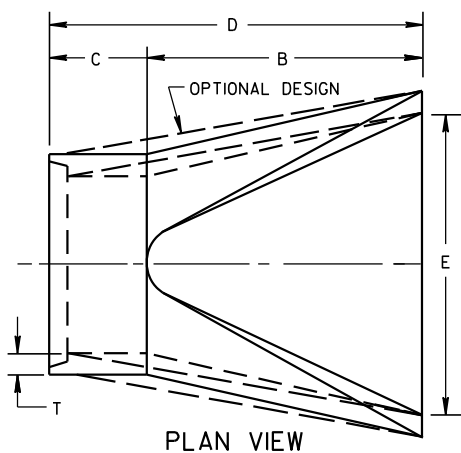
APRON ENDWALLS FOR
CULVERT PIPE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

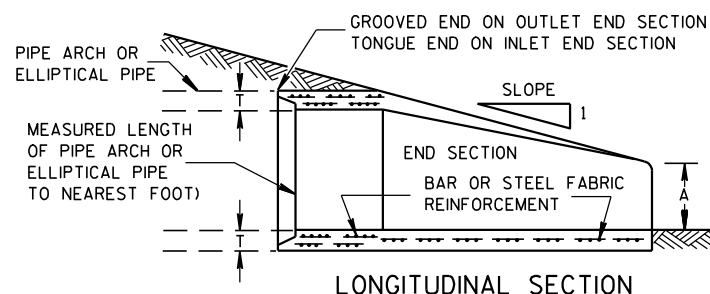
APPROVED
11/30/94 /S/ Rory L. Rhinesmith
DATE CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA



END VIEW



PLAN VIEW



LONGITUDINAL SECTION

CONCRETE ENDWALLS

2- 2 2/3" X 1/2" CORRUGATIONS

EQUIV. DIA. (Inches)	(Inches)		MIN. THICK. (Inches)		DIMENSIONS (Inches)							APPROX. SLOPE	BODY
	SPAN	RISE	STEEL	ALUM.	A (±1")	B (MAX.)	H (±1")	L (±1 1/2")	L1 (⓪)	L2 (⓪)	W (±2")		
15	17	13	.064	.060	7	9	6	19	14	16	30	2 1/2 to 1	1 Pc.
18	21	15	.064	.060	7	10	6	23	14	19 3/8	36	2 1/2 to 1	1 Pc.
21	24	18	.064	.060	8	12	6	28	18	21 3/4	42	2 1/2 to 1	1 Pc.
24	28	20	.064	.060	9	14	6	32	18	27 1/2	48	2 1/2 to 1	1 Pc.
30	35	24	.079	.075	10	16	6	39	18	37 5/8	60	2 1/2 to 1	1 Pc.
36	42	29	.079	.075	12	18	8	46	24	45 3/8	75	2 1/2 to 1	1 Pc.
42	49	33	.109	.105	13	21	9	53	24	54 3/4	85	2 1/2 to 1	2 Pc.
48	57	38	.109	.105	18	26	12	63	24	68	90	2 1/2 to 1	3 Pc.
54	64	43	.109	.105	18	30	12	70	24	72 3/4	102	2 1/4 to 1	3 Pc.
60	71	47	.109*	.105*	18	33	12	77	30	82 1/4	114	2 1/4 to 1	3 Pc.
66	77	52	.109*	.105*	18	36	12	77	—	—	126	2 to 1	3 Pc.
72	83	57	.109*	.105*	18	39	12	77	—	—	138	2 to 1	3 Pc.

3" X 1" CORRUGATIONS

EQUIV. DIA. (Inches)	(Inches)		MIN. THICK. (Inches)		DIMENSIONS (Inches)							APPROX. SLOPE	BODY
	SPAN	RISE	STEEL	ALUM.	A (±1")	B (MAX.)	H (±1")	L (±1 1/2")	L1 (⓪)	L2 (⓪)	W (±2")		
48	53	41	.109	.105	18	26	12	63	24	72 3/4	90	2 1/2 to 1	2 Pc.
54	60	46	.109	.105	18	30	12	70	30	82 1/4	102	2 to 1	2 Pc.
60	66	51	.109*	.105*	18	33	12	77	—	—	114	1 1/2 to 1	3 Pc.
66	73	55	.109*	.105*	18	36	12	77	—	—	126	1 1/2 to 1	3 Pc.
72	81	59	.109*	.105*	18	39	12	77	—	—	138	2 to 1	3 Pc.
78	87	63	.109*	.105*	22	38	12	77	—	—	148	1 1/2 to 1	3 Pc.
84	95	67	.109*	.105*	22	34	12	77	—	—	162	1 1/2 to 1	3 Pc.
90	103	71	.109*	.105*	22	38	12	77	—	—	174	1 1/2 to 1	3 Pc.
96	112	75	.109*	.105*	24	40	12	77	—	—	174	1 1/2 to 1	3 Pc.

NOTE: ALL SPLICES TO BE LAP RIVETED OR BOLTED. * EXCEPT CENTER PANEL SEE GENERAL NOTES

REINFORCED CONCRETE PIPE ARCH

EQUIV. DIA. (Inches)	DIMENSIONS (Inches)								APPROX. SLOPE
	**SPAN	**RISE	T	A	B	C	D	E	
24	29	18	3	8 1/2	39	33	72	48	3 to 1
30	36	22	3 1/2	9 1/2	50	46	96	60	3 to 1
36	44	27	4	11 1/8	60	36	96	72	3 to 1
42	51	31	4 1/2	15 1/8	60	36	96	78	3 to 1
48	58	36	5	21	60	36	96	84	3 to 1
54	65	40	5 1/2	25 1/2	60	36	96	90	3 to 1
60	73	45	6	31	60	36	96	96	3 to 1
72	88	54	7	31	60	39	99	120	2 to 1
84	102	62	8	28 1/2	83	19	102	144	2 to 1

REINFORCED CONCRETE ELLIPTICAL PIPE

EQUIV. DIA. (Inches)	DIMENSIONS (Inches)								APPROX. SLOPE
	**SPAN	**RISE	T	A	B	C	D	E	
24	30	19	3 1/4	8 1/2	39	33	72	48	3 to 1
30	38	24	3 3/4	9 1/2	54	18	72	60	3 to 1
36	45	29	4 1/2	11 1/8	60	24	84	72	2 1/2 to 1
42	53	34	5	15 3/4	60	36	96	78	2 1/2 to 1
48	60	38	5 1/2	21	60	36	96	84	2 1/2 to 1
54	68	43	6	25 1/2	60	36	96	90	2 1/2 to 1
60	76	48	6 1/2	30	60	36	96	96	2 1/2 to 1

**NOMINAL SIZE

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

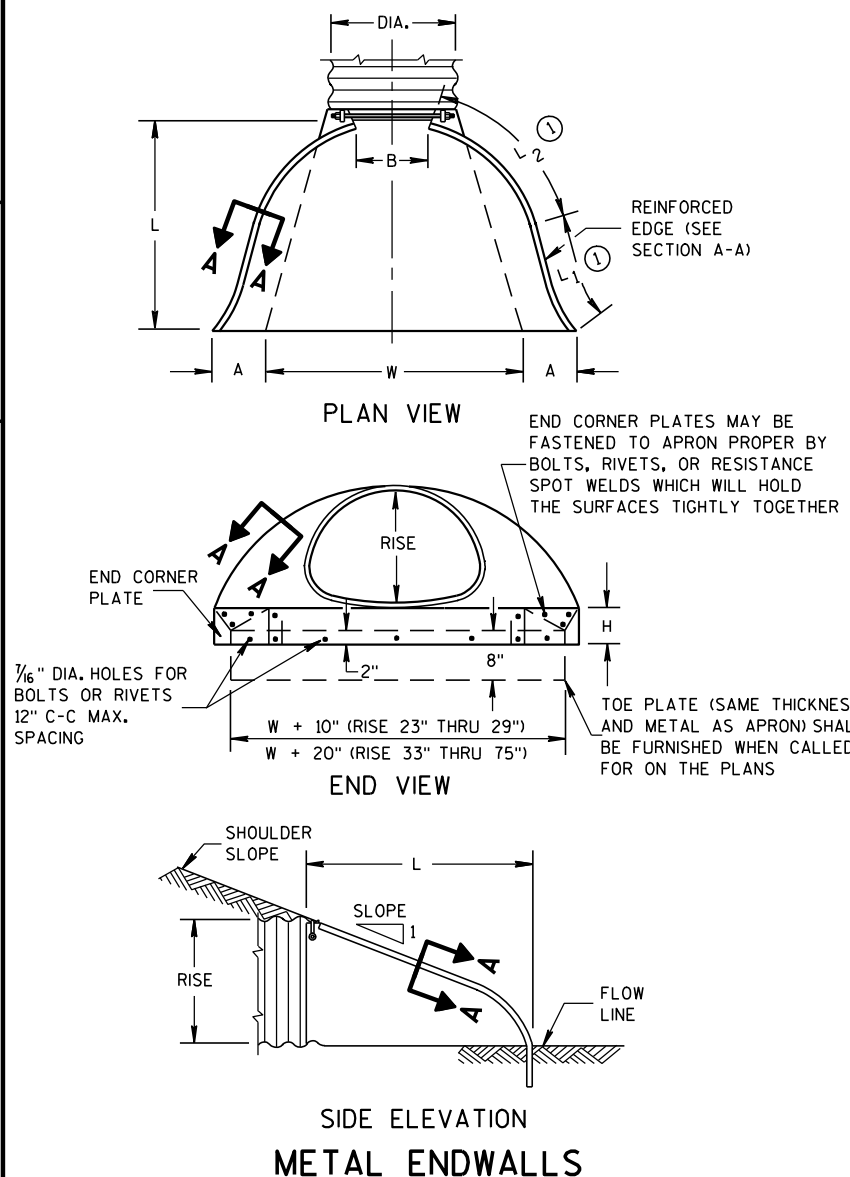
CONCRETE APRON ENDWALLS MAY NOT BE USED WITH GALVANIZED STEEL OR ALUMINUM CULVERT PIPE OR VISE VERSA. GALVANIZED STEEL OR ALUMINUM APRON ENDWALLS SHALL NORMALLY BE INSTALLED ON CULVERT PIPE OF THE SAME METAL.

ALL THREE PIECE STEEL APRON ENDWALLS FOR 66" X 51" PIPE ARCH AND LARGER SHALL HAVE 0.109" SIDES AND 0.138" CENTER PANELS. ALL THREE PIECE ALUMINUM APRON ENDWALLS FOR 66" X 51" PIPE ARCH AND LARGER SHALL HAVE 0.105" SIDES AND 0.134" CENTER PANELS. THE WIDTH OF CENTER PANELS SHALL BE GREATER THAN 20 PERCENT OF THE PIPE ARCH PERIMETER.

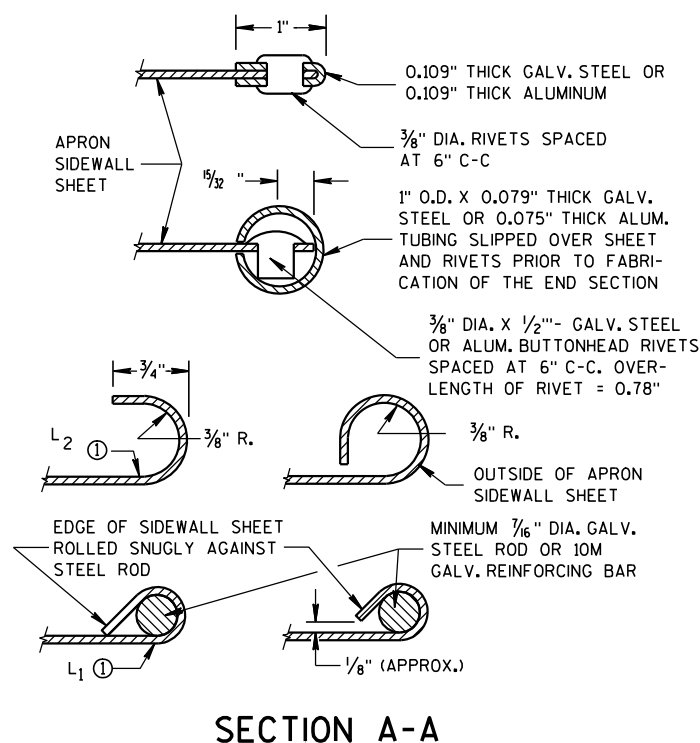
LAP SEAMS SHALL BE TIGHTLY JOINED BY GALVANIZED RIVETS OR BOLTS FOR STEEL UNITS AND ALUMINUM RIVETS AND BOLTS FOR ALUMINUM UNITS. FOR THE 77" X 52" THROUGH 112" X 75" APRON ENDWALL SIZES, THE REINFORCED EDGES AND CENTER PANEL SEAMS SHALL BE FURTHER REINFORCED WITH GALVANIZED STEEL OR ALUMINUM STIFFENER ANGLES. THE ANGLES SHALL BE ATTACHED BY GALVANIZED NUTS AND BOLTS FOR STEEL UNITS AND ALUMINUM NUTS AND BOLTS FOR ALUMINUM UNITS.

WHERE TWO OR MORE PIPES WITH APRON ENDWALLS ARE LAID ADJACENT TO EACH OTHER, THEY SHALL BE SEPARATED BY A DISTANCE SUFFICIENT TO PROVIDE A MINIMUM CLEARANCE OF 6 INCHES BETWEEN APRON ENDWALLS.

⓪ FOR PIPE ARCH SIZES UP TO 73" X 55" A 180° ROLLED EDGE MAY BE USED INSTEAD OF STEEL ROD REINFORCEMENT. SEE SECTION A-A.



METAL ENDWALLS

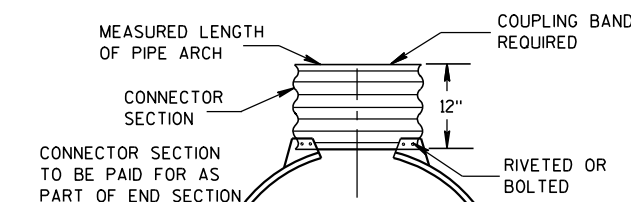


SECTION A-A



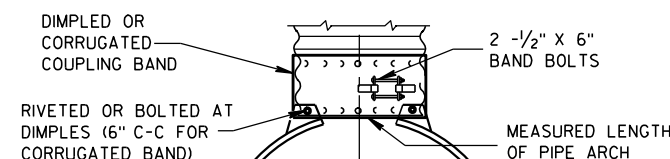
TYPE 2

FOR 17" X 13" THRU 112" X 75" PIPE ARCH



TYPE 3

FOR 64" X 43" THRU 112" X 75" PIPE ARCH



TYPE 5

ALTERNATE FOR: ALL SIZES CORRUGATED PIPE ARCHES

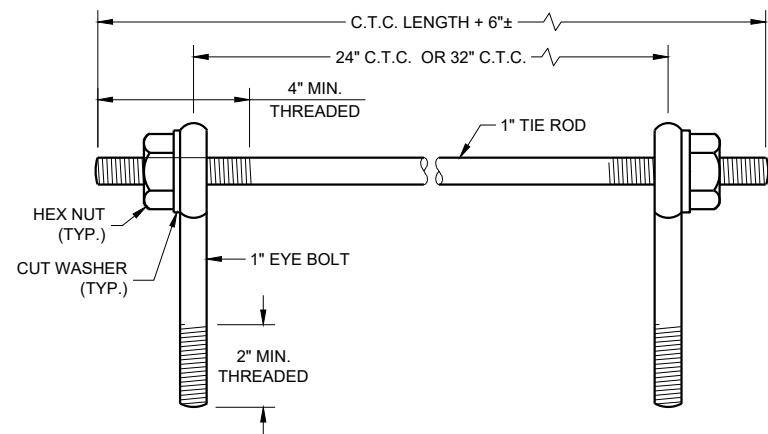
NOTE: DIMPLED BAND FITS OVER OUTSIDE OF ENDWALL, AND CORRUGATED BAND FITS INSIDE ENDWALL.

CONNECTION DETAILS

APRON ENDWALLS FOR PIPE ARCH AND ELLIPTICAL PIPE

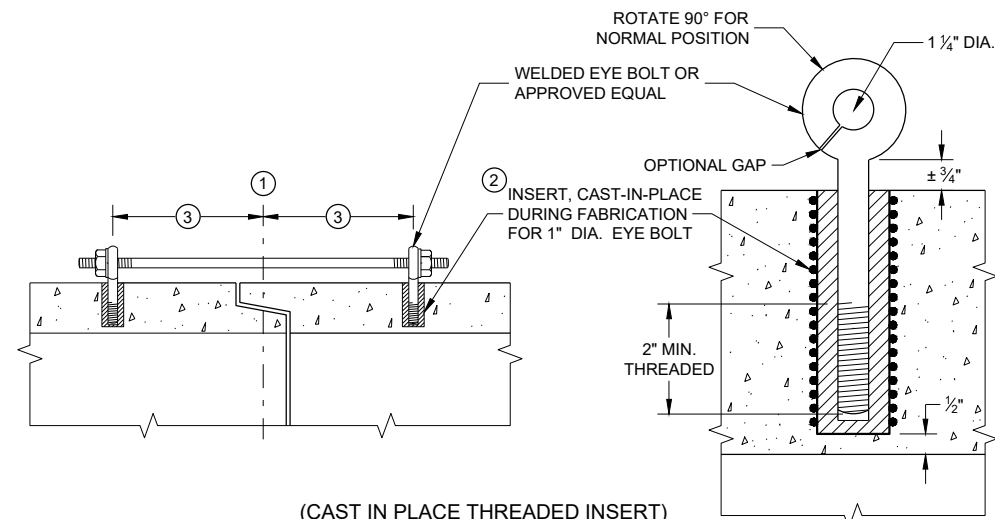
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
11/30/94 /S/ Rory L. Rhinesmith
DATE CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA



EYE BOLTS AND TIE ROD

EYE BOLT AND TIE ROD ASSEMBLY (ALTERNATE NO. 1)



(CAST IN PLACE THREADED INSERT)
LONGITUDINAL SECTIONS

GENERAL NOTES

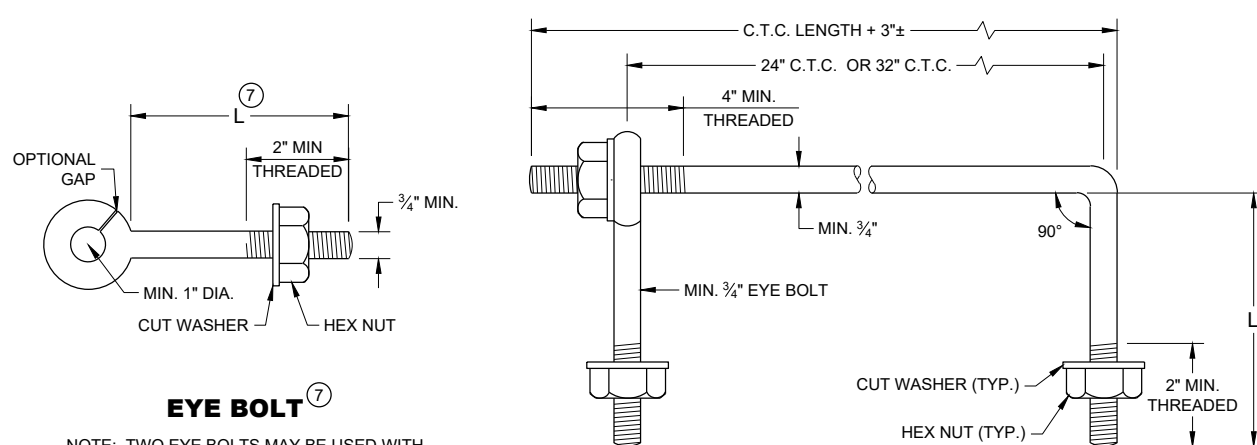
DETAILS OF CONSTRUCTION, MATERIALS, AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

CONCRETE CULVERT AND STORM SEWER PIPE SHALL BE TIED TOGETHER IN THE MANNER ILLUSTRATED BY THIS DETAIL AT LOCATIONS DESIGNATED IN THE STANDARD SPECIFICATIONS AND THE PLAN. THE CONTRACTOR MAY USE EITHER ALTERNATE 1, 2 OR 3 FOR DRAINAGE STRUCTURES. ONLY ALTERNATE 1 AND 3 MAY BE USED FOR CATTLE PASSES, UNLESS OTHERWISE STATED IN THE CONTRACT. THE MATERIALS, FABRICATION AND WORK NECESSARY TO TIE THE PIPE BY THIS DETAIL WILL BE CONSIDERED INCIDENTAL TO THE PIPE AND APRON ENDWALLS IF REQUIRED.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR JOINT TIES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.

JOINT TIES TO BE HOT-DIP GALVANIZED PER ASTM A 153.

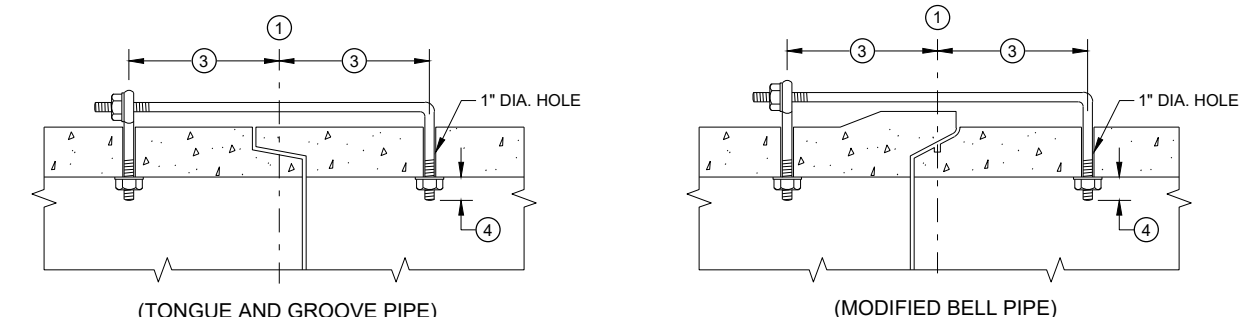
- ① CENTER LINE OF TONGUE AND GROOVE OR BELL AND SPIGOT JOINTS.
- ② THE INSIDE OF THE THREADED INSERTS SHALL BE CLEAN TO ALLOW THE INSERTION OF THREADED EYE BOLTS.
- ③ HOLES SHALL BE CAST-IN-PLACE OR DRILLED PER THE APPLICABLE DETAIL, AND EQUAL DISTANCE FROM THE CENTERLINE OF THE JOINT.
- ④ BOLT PROJECTION INSIDE OF PIPE SHALL NOT EXCEED 2 INCHES.
- ⑤ OPENING TO BE ROD DIAMETER PLUS 1 INCH.
- ⑥ LENGTH ADEQUATE TO EXTEND TO WITHIN 1/2 INCH OF THE INNER SURFACE OF THE PIPE.
- ⑦ EYE BOLT LENGTH DETERMINED BY WALL THICKNESS, BELL THICKNESS AND BOLT PROJECTION INSIDE PIPE.



EYE BOLT ⑦

NOTE: TWO EYE BOLTS MAY BE USED WITH A 30\"/>

EYE BOLT AND TIE ROD



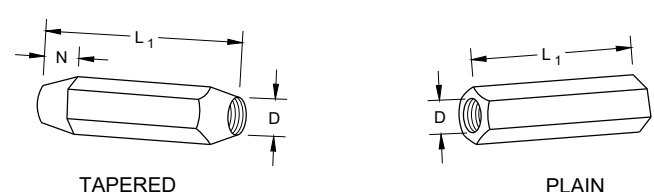
LONGITUDINAL SECTION
(JOINT TIES FOR 18\"/>

EYE BOLT AND TIE ROD ASSEMBLY (ALTERNATE NO. 2)

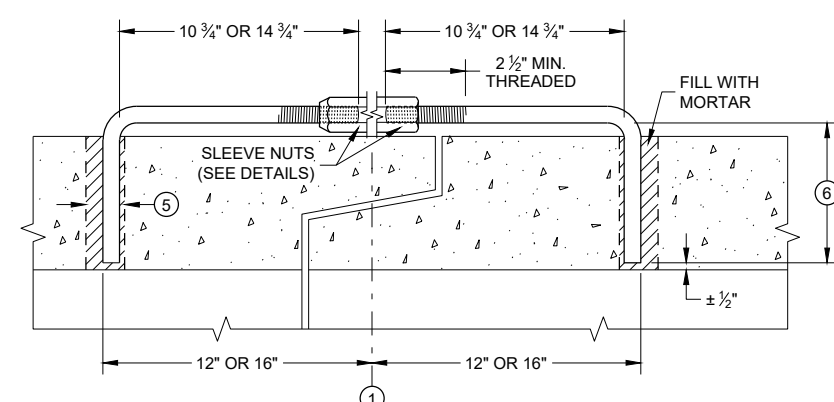
ADJUSTABLE TIE ROD TABLE

PIPE DIAMETER	TIE ROD DIAMETER	D	L ₁	N
12 - 60	5/8	5/8	5	1/2
66 - 84	3/4	3/4	5	1/2
90 - 144	1	1	7	1 7/16

DIMENSIONS SHOWN ARE IN INCHES

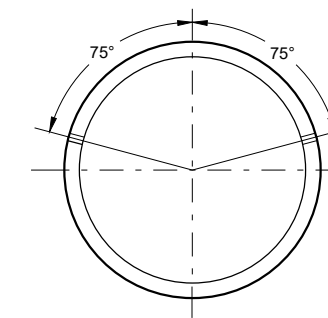


RIGHT AND LEFT THREADS SLEEVE NUTS



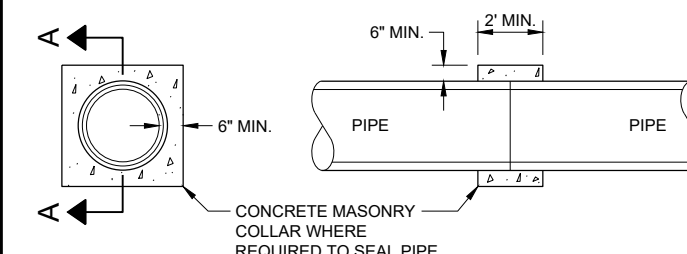
LONGITUDINAL SECTION

ADJUSTABLE TIE ROD (ALTERNATE NO. 3)



PLACEMENT OF (2) CAST-IN-PLACE INSERTS OR HOLES DURING FABRICATION FOR PIPE SECTIONS REQUIRING TIE RODS

TRANSVERSE SECTION



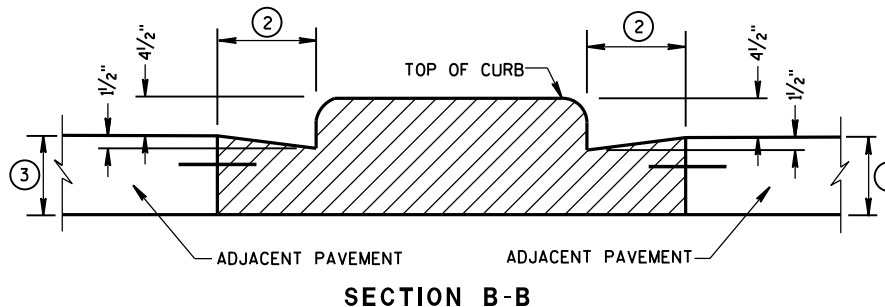
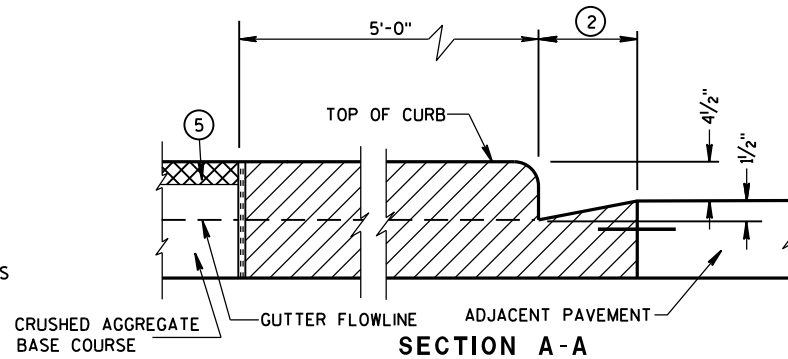
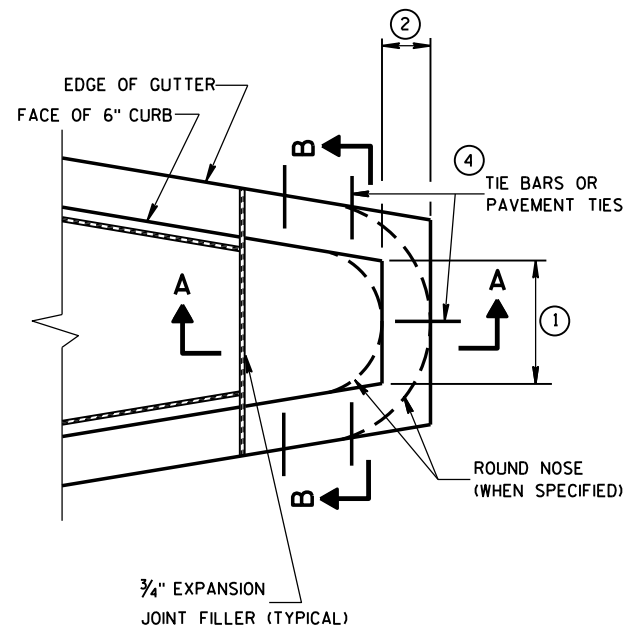
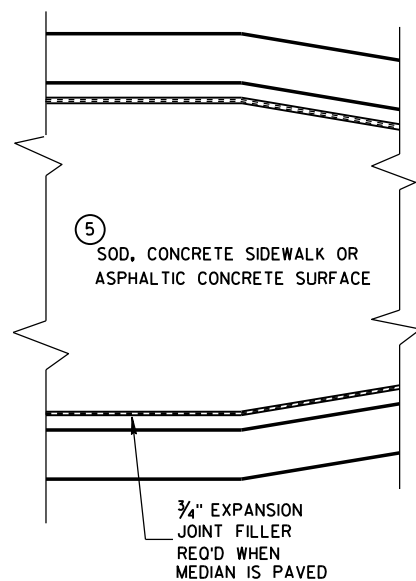
SECTION A - A
CONCRETE COLLAR DETAIL

JOINT TIES FOR CONCRETE PIPE AND CONCRETE COLLAR DETAIL

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2021 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
ENGINEER

FHWA

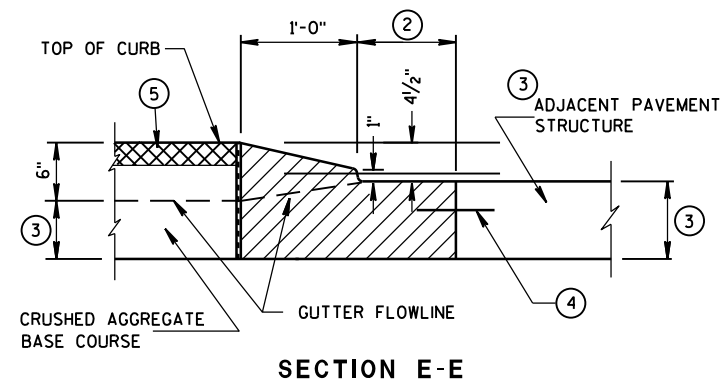
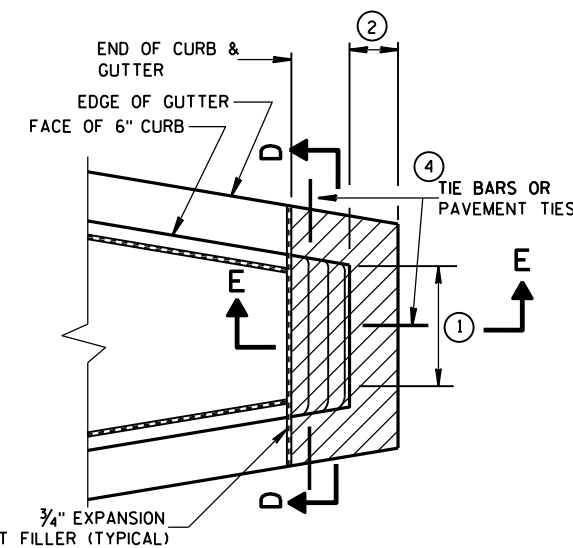


CONCRETE MEDIAN BLUNT NOSE DETAIL

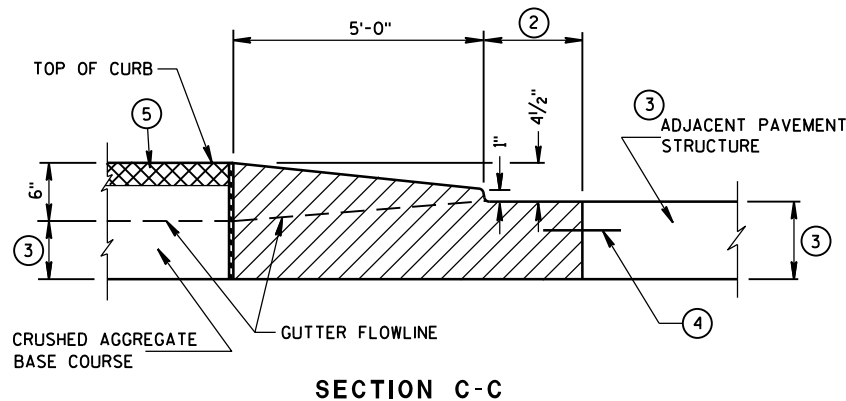
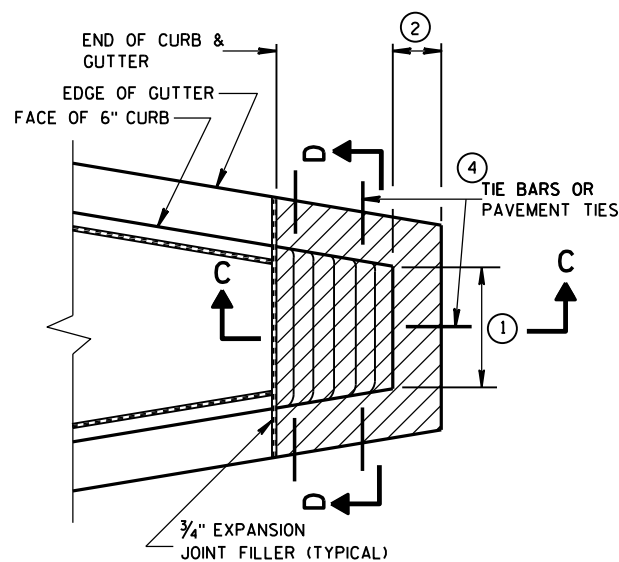
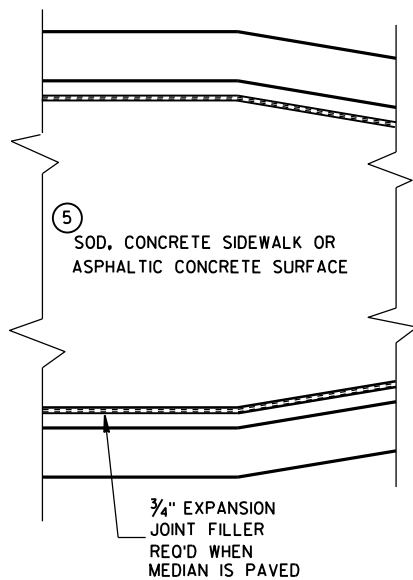
GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

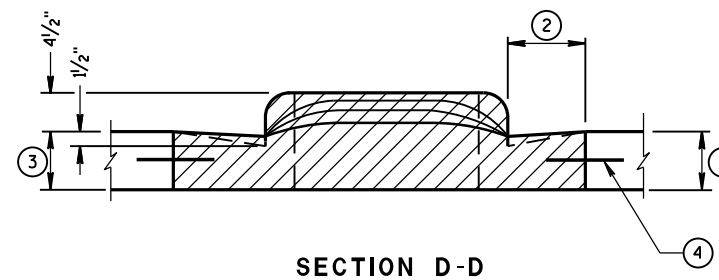
- ① SEE PLAN FOR MEDIAN NOSE WIDTH AND RADIUS (FOR ROUND NOSE ALTERNATE).
- ② WIDTH OF GUTTER TO MATCH EXISTING ADJACENT GUTTER OR AS SPECIFIED ELSEWHERE IN THE PLAN.
- ③ DEPTH EQUAL TO ADJACENT PAVEMENT. ADJACENT PAVEMENT STRUCTURE DETAILS ARE SHOWN ON THE PLAN. TYPICAL OPTIONS ARE:
 - (1) NEW OR EXISTING CONCRETE PAVEMENT.
 - (2) ASPHALTIC CONCRETE PAVEMENT OVER NEW OR EXISTING CONCRETE BASE COURSE.
 - (3) ASPHALTIC CONCRETE PAVEMENT OVER CRUSHED AGGREGATE BASE COURSE.
- ④ TIE BARS OR PAVEMENT TIES REQUIRED IN NEW CONCRETE PAVEMENT OR CONCRETE BASE COURSE. TIE BARS SHALL BE NO. 4 X 2'-0" SPACED AT 2'-0" C-C.
- PAVEMENT TIES REQUIRED IN EXISTING CONCRETE BASE COURSE. PAVEMENT TIES SHALL BE NO. 6 X 1'-0" SPACED AT 3'-0" C-C INSTALLED ON A HORIZONTAL SKEW OF 6:1. THE DIRECTION OF SKEW SHALL ALTERNATE AFTER EVERY ONE OR TWO BARS.
- ⑤ SURFACE TYPE AND DETAILS ARE SHOWN ELSEWHERE IN THE PLAN.



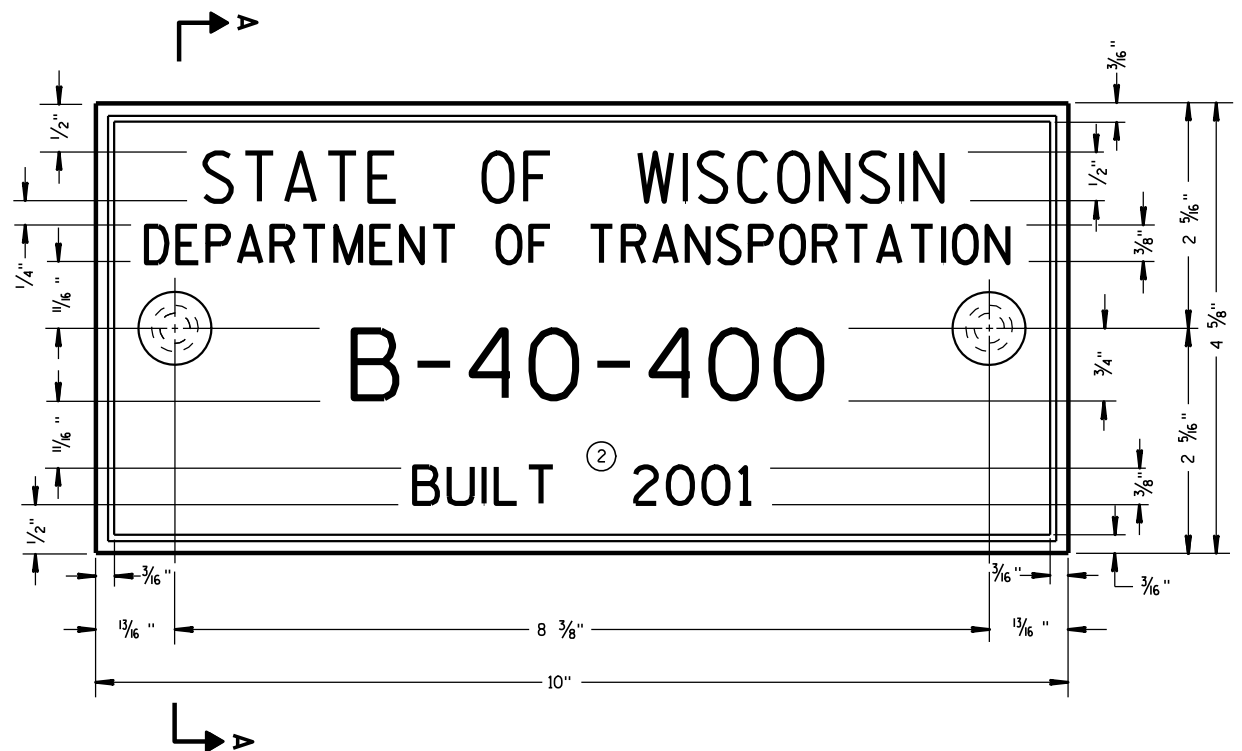
CONCRETE MEDIAN SLOPED NOSE TYPE 2



CONCRETE MEDIAN SLOPED NOSE TYPE 1



CONCRETE MEDIAN NOSE	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 6/8/2006 DATE	/s/ Jerry H. Zogg ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA	



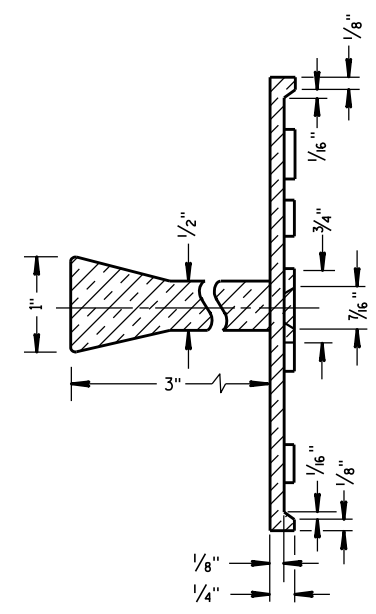
TYPICAL NAME PLATE
(BRIDGES, CULVERTS, AND RETAINING WALLS)

GENERAL NOTES

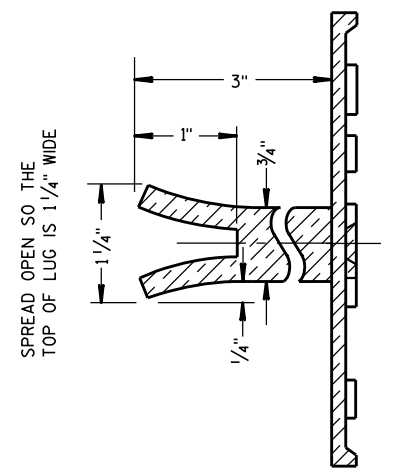
NAME PLATES TO BE INSTALLED ON BRIDGES, CULVERTS, AND RETAINING WALLS SHALL CONFORM TO THE REQUIREMENTS OF SECTION 502.3.11 OF THE STANDARD SPECIFICATIONS.

THE BRIDGE NUMBER AND YEAR BUILT SHOWN ON THIS DRAWING ARE EXAMPLES ONLY. SEE CONSTRUCTION PLANS FOR INDIVIDUAL NUMBERING AND YEAR BUILT.

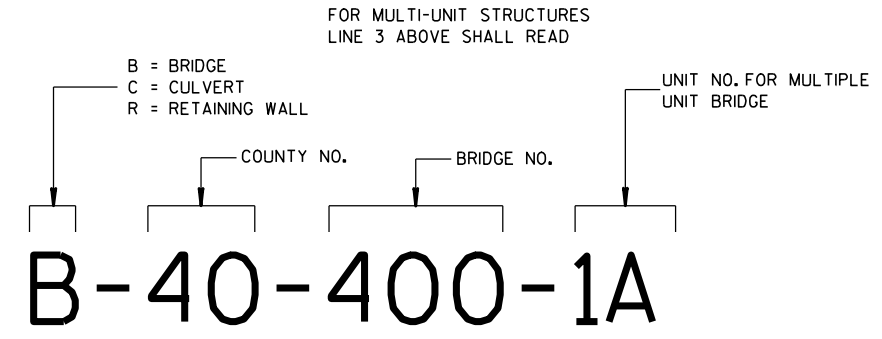
- ① EPOXY RESIN SHALL BE FROM AN APPROVED MANUFACTURER AND USED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS.
- ② REHABILITATION OF AN EXISTING STRUCTURE SHOULD USE THE DATE OF ORIGINAL STRUCTURE CONSTRUCTION.



SECTION A-A

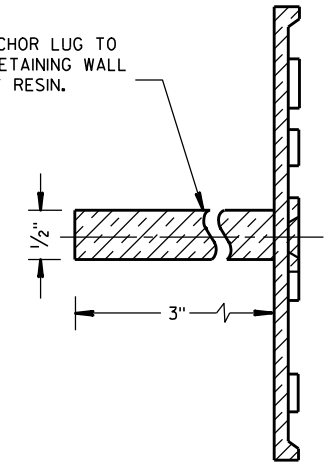


ALTERNATE LUG



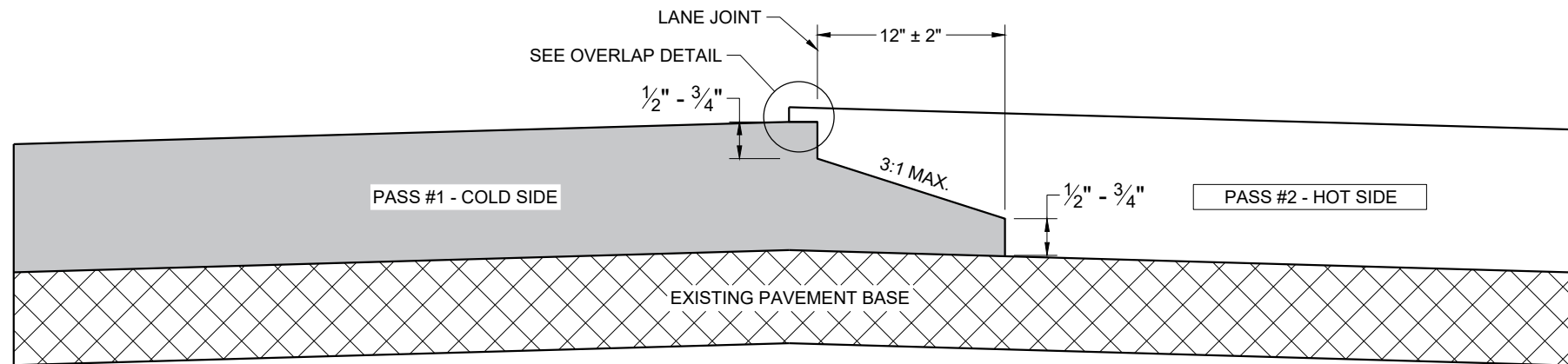
**NUMBERING DESIGNATION
MULTI-UNIT STRUCTURES**

- ① ADHERE ANCHOR LUG TO PRECAST RETAINING WALL WITH EPOXY RESIN.

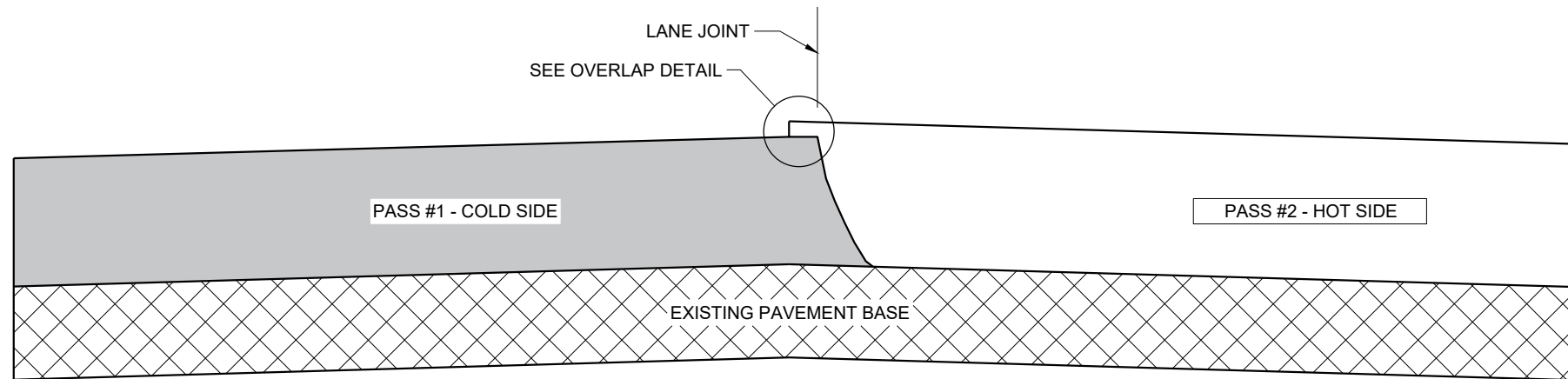


ALTERNATE LUG
(FOR ATTACHMENT TO PRECAST STRUCTURES)

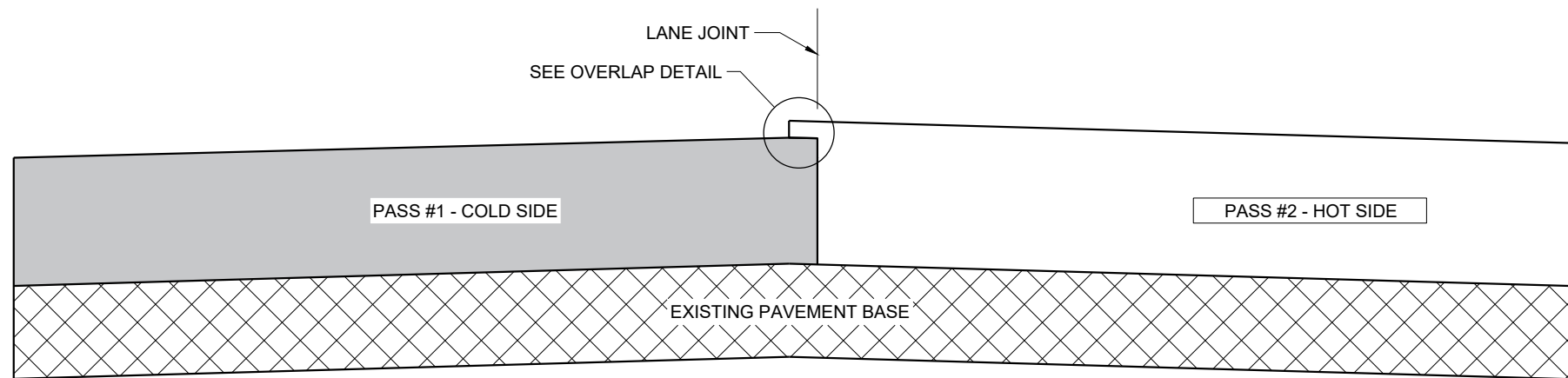
NAME PLATE (STRUCTURES)	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED DATE 3/26/10	/S/ Scot Becker CHIEF STRUCTURAL DEVELOPMENT ENGINEER
FHWA	



**TYPICAL PAVEMENT CROSS SECTION
NOTCHED WEDGE JOINT**



**TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT**



**TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT (MILLED)**

GENERAL NOTES

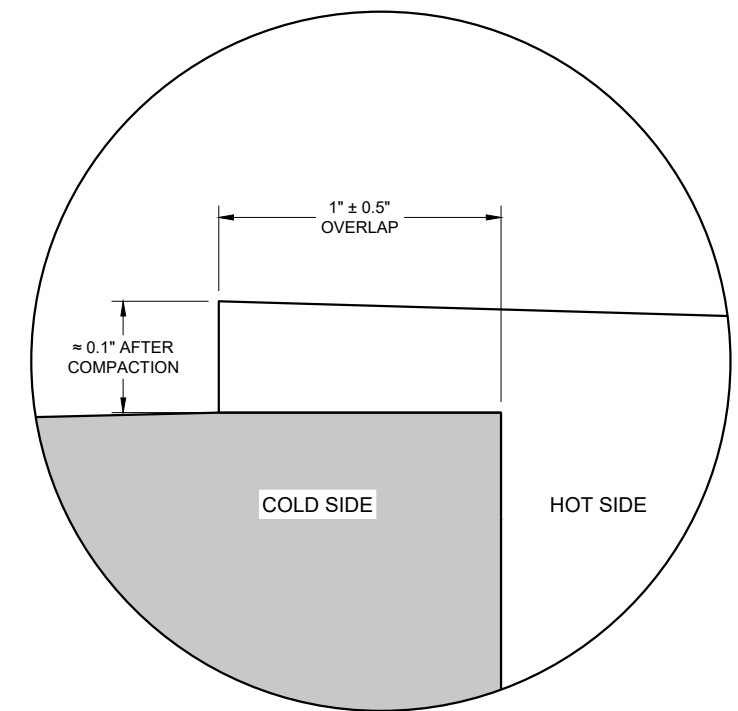
IN ADDITION TO THE DETAILS PROVIDED IN THIS DRAWING, CONFORM TO STANDARD SPECIFICATION 450.3.2.8 FOR WHEN A NOTCHED WEDGE JOINT IS REQUIRED AND FOR GENERAL JOINT CONSTRUCTION REQUIREMENTS.

FOR ALL LONGITUDINAL JOINTS, ENSURE THE PAVER SCREED OVERLAPS THE PREVIOUSLY PLACED PAVEMENT BY $1" \pm 0.5"$ AND THE HOT SIDE OF THE JOINT REMAINS HIGHER THAN THE COLD SIDE BY APPROXIMATELY $0.1"$ AFTER FINAL COMPACTION. (IT WILL BE FLUSH WHEN PAVING IN ECHELON.)

ONLY REMOVE THE LONGITUDINAL NOTCHED WEDGE JOINT FOR SMA PAVEMENT OR AS DIRECTED BY THE ENGINEER TO ADDRESS SPECIFIC LENGTHS OF JOINT DAMAGED BY TRAFFIC.

WHEN MILLING BACK OR REMOVING ANY LONGITUDINAL JOINT, LIMIT THE MATERIAL REMOVED TO $2"$ FROM THE TOP NOTCH OR FROM THE VERTICAL JOINT EDGE ON THE COLD SIDE OF THE JOINT.

USE LONGITUDINAL MILLED JOINT AS PLANS SHOW OR THE AS THE ENGINEER DIRECTS.



OVERLAP DETAIL (TYPICAL)

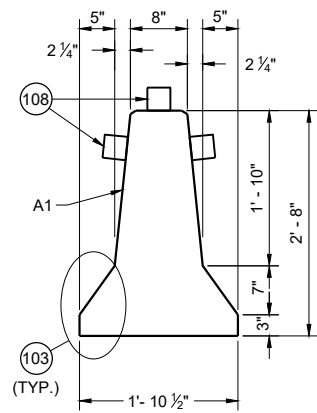
6

6

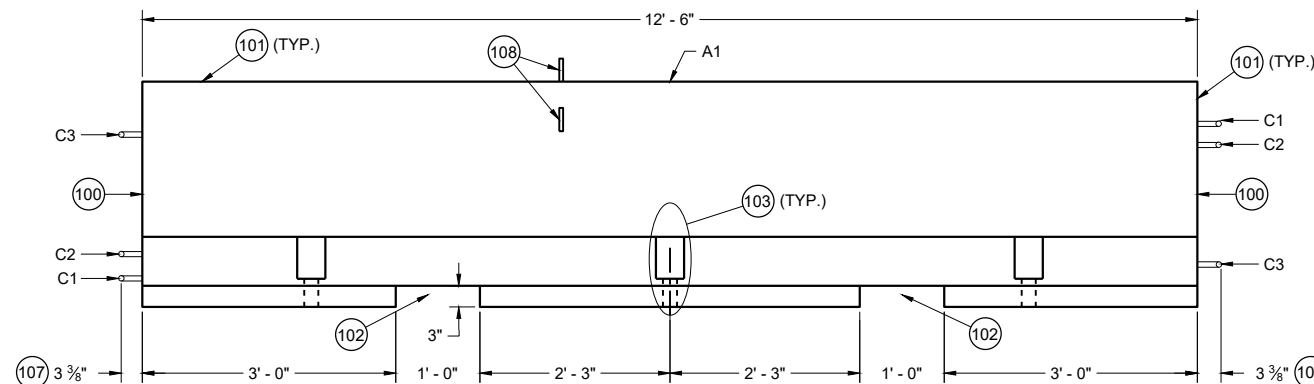
SDD 13C19 - 03

SDD 13C19 - 03

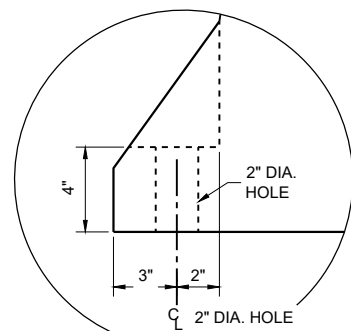
HMA LONGITUDINAL JOINTS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED November 2020 DATE	/S/ Steven Hefel HMA PAVEMENT ENGINEER
FHWA	



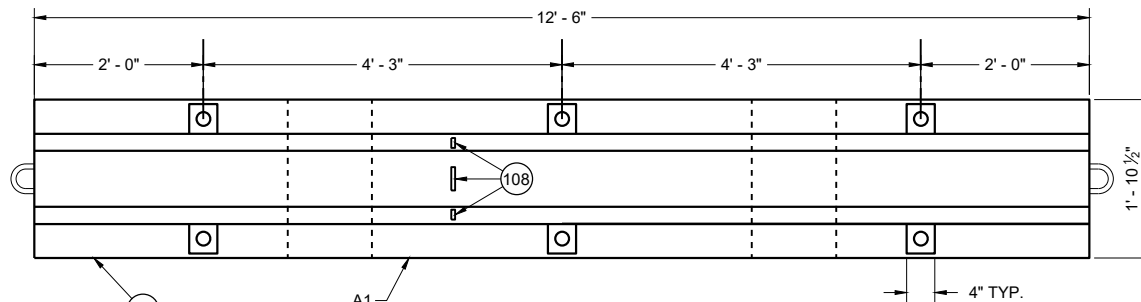
CROSS SECTION



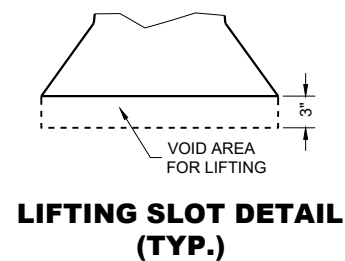
PROFILE VIEW



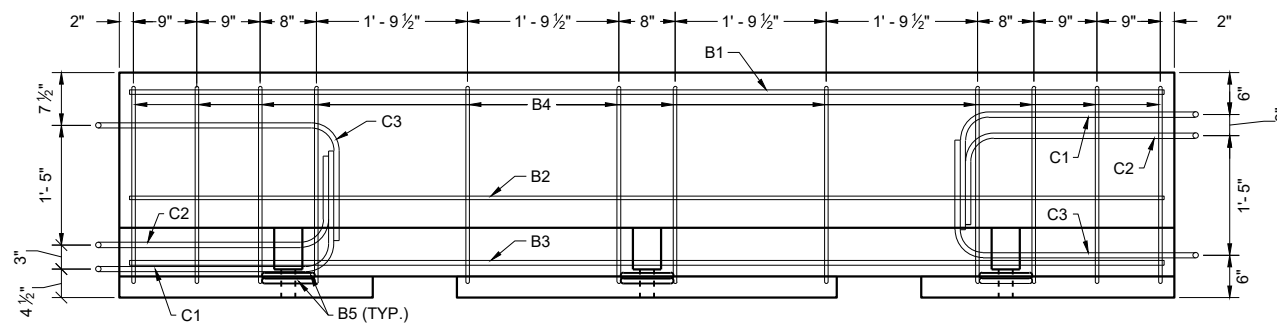
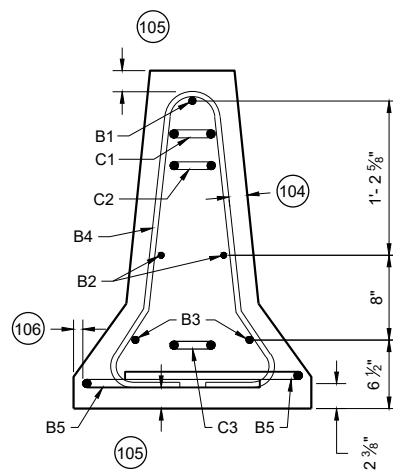
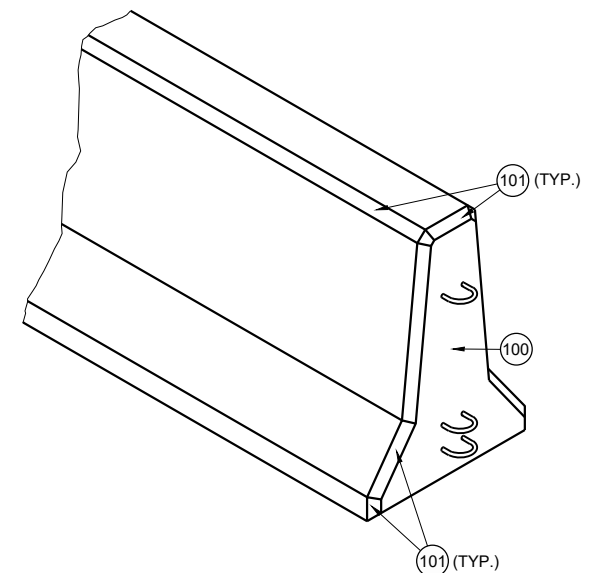
ANCHOR BLOCK DETAIL



**PLAN VIEW
TEMPORARY BARRIER**



**LIFTING SLOT DETAIL
(TYP.)**



**PROFILE VIEW
TEMPORARY BARRIER REINFORCEMENT**

GENERAL NOTES

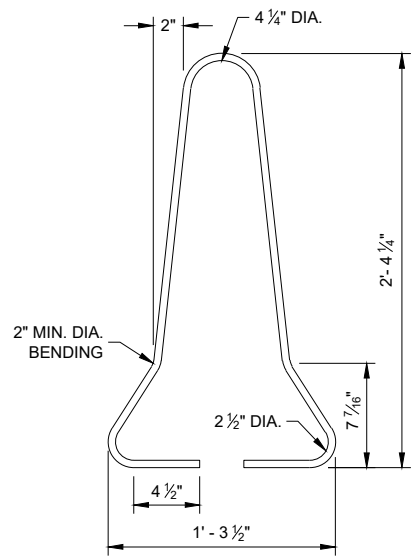
PLACE BARRIER ON PAVED SURFACE. BEFORE PLACEMENT OF TEMPORARY BARRIER, REMOVE ALL LOOSE MATERIAL FROM PAVED SURFACE.

LOOP BARS C1, C2 AND C3 ARE NOT FOR PLACEMENT OR MOVEMENT OF BARRIER.

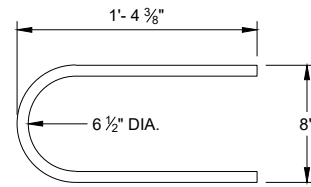
- (100) PERMANENTLY FORM INTO ONE END OF BARRIER THE FOLLOWING INFORMATION:
A. TYPE OF BARRIER: WI-CBTP
B. MANUFACTURER
C. DATE OF MANUFACTURE (MONTH AND YEAR)
- (101) 1" OPTIONAL CHAMFER
- (102) SEE LIFTING SLOT DETAIL
- (103) SEE ANCHOR BLOCK DETAIL
- (104) 1 3/4" MIN. CLEAR COVER
- (105) 2" MIN. CLEAR COVER
- (106) 1" MIN. CLEAR COVER
- (107) ± 1/8" MEASURED FROM FACE OF CONCRETE BARRIER TO OUTSIDE OF LOOP BAR (TYP.)
- (108) USE DELINEATORS CONFORMING TO SECTION 633 OF THE STANDARD SPECIFICATIONS. CONTRACTOR MAY USE ALTERNATE SHAPES AND HOUSING. INSTALL DELINEATORS ACCORDING TO MANUFACTURER'S INSTRUCTION. INSTALL YELLOW REFLECTORS WHEN BARRIER IS LOCATED LEFT OF TRAFFIC AND WHITE WHEN BARRIER IS LOCATED RIGHT OF TRAFFIC. SPACE DELINEATORS A MAXIMUM OF 25 FEET APART. PROVIDE TOP MOUNTED DELINEATORS IN ADDITION TO SIDE MOUNTED DELINEATORS ON BARRIER INSTALLATIONS LOCATED ON A CURVED ALIGNMENT LONGER THAN 200 FEET AND ON BARRIERS USED TO SEPARATE OPPOSING TRAFFIC.

**CONCRETE BARRIER
TEMPORARY PRECAST,
12' - 6"**

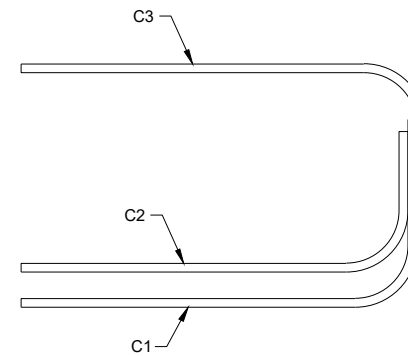
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



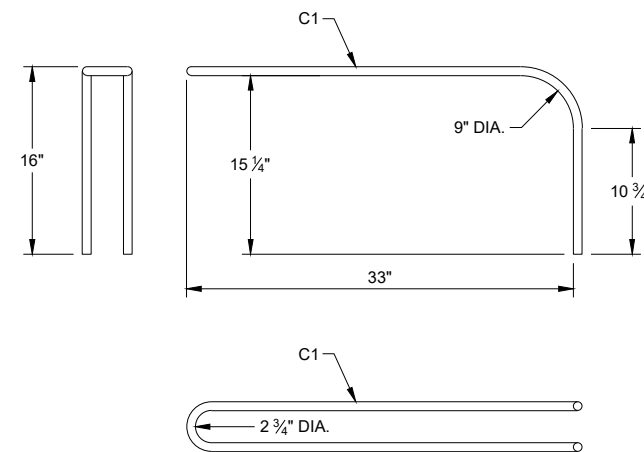
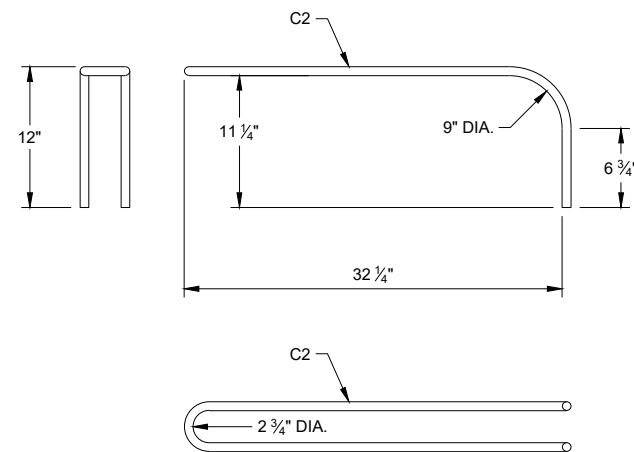
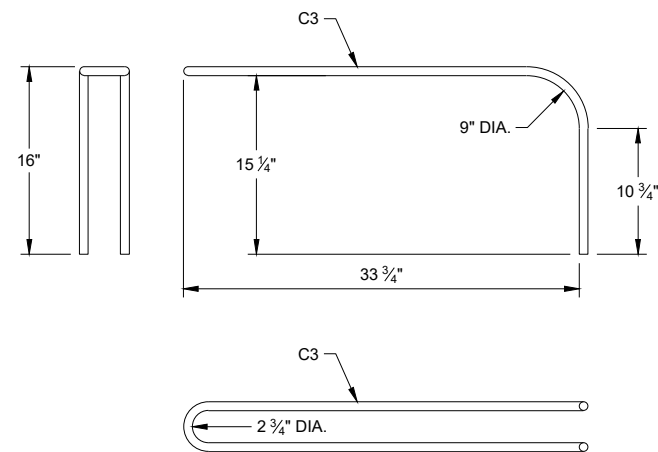
B4 BAR DETAIL



B5 BAR DETAIL



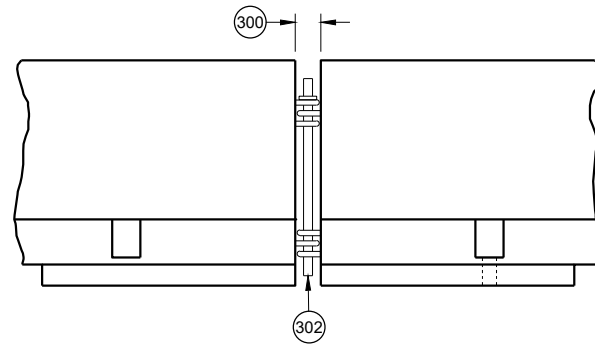
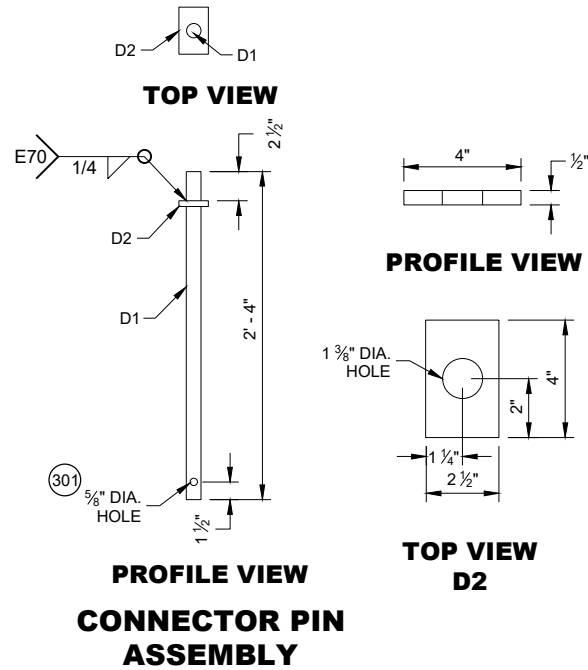
**PROFILE VIEW
LOOP BAR ASSEMBLY**



C BAR DETAILS

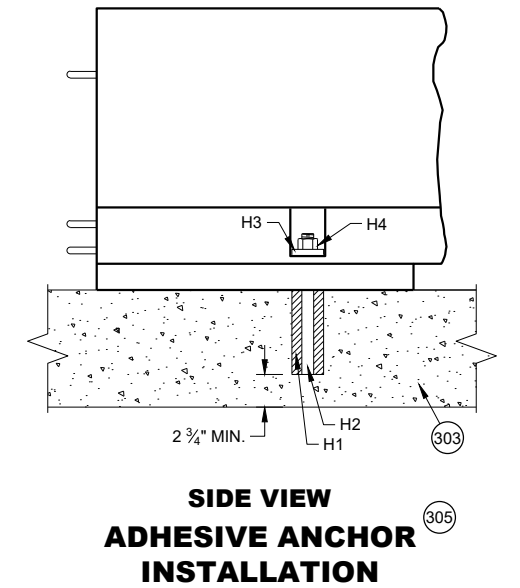
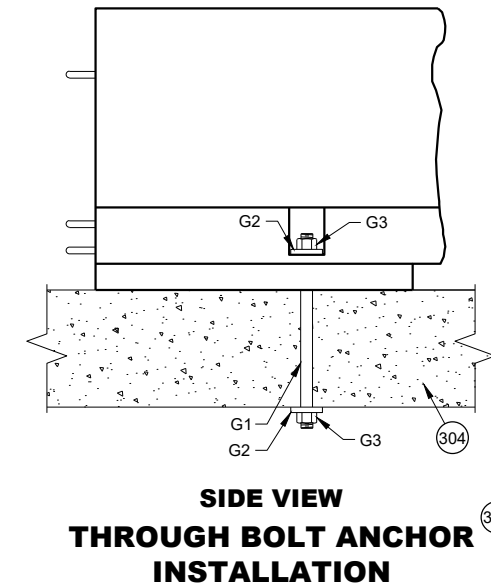
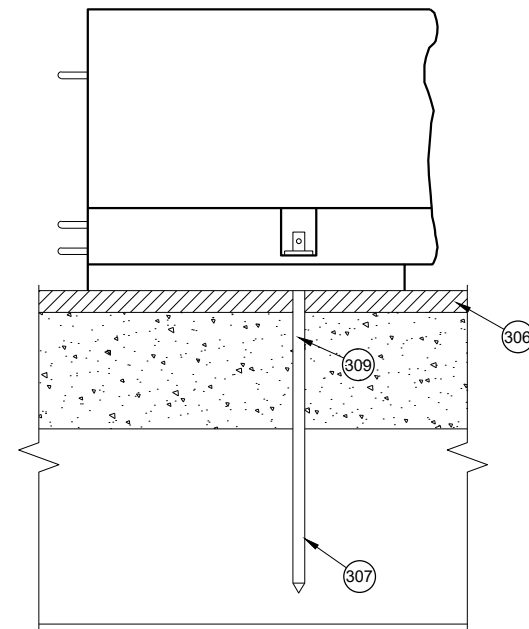
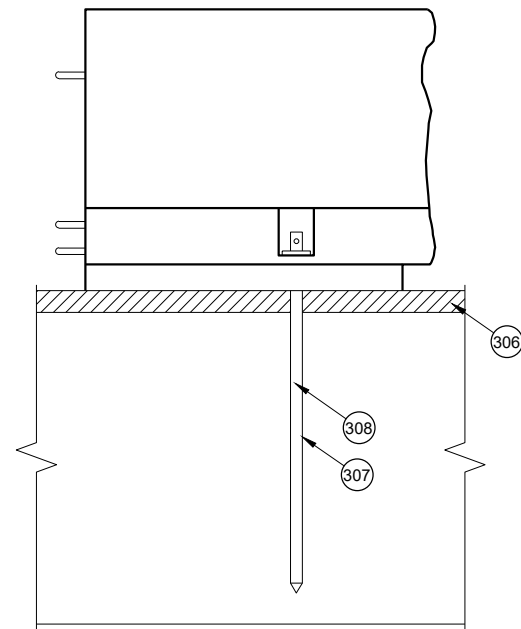
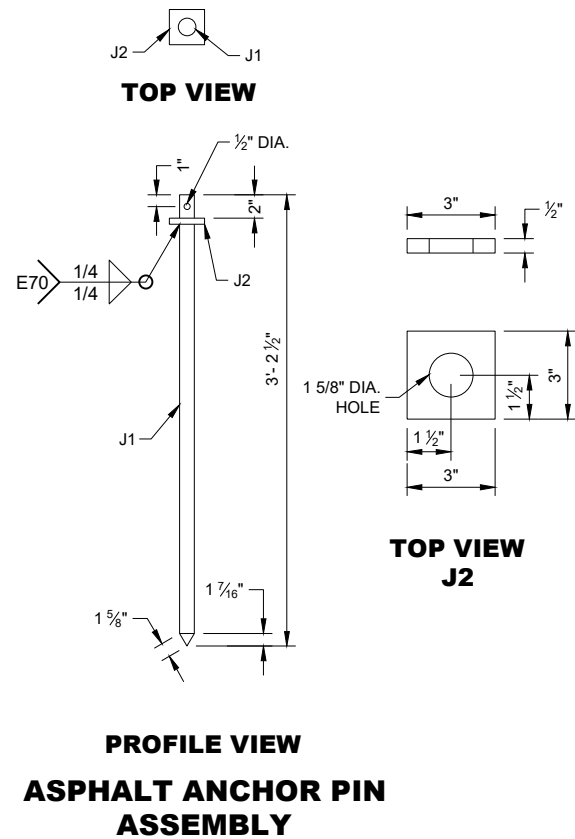
**CONCRETE BARRIER
TEMPORARY PRECAST,
12' - 6"**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



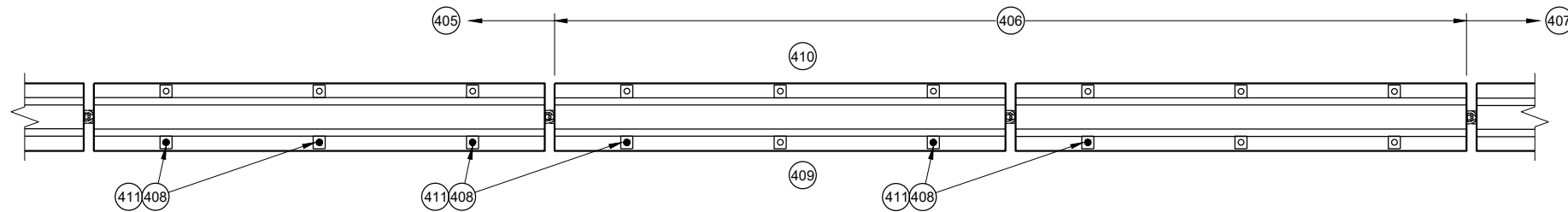
GENERAL NOTES

- (300) SET WITH 3 5/8" WOOD BLOCK.
- (301) HOLE IS OPTIONAL.
- (302) CONNECTOR PIN ASSEMBLY.
- (303) CONCRETE PAVEMENT, APPROACH SLAB, OR DECK.
- (304) CONCRETE DECK.
- (305) DO NOT USE ON CONCRETE BRIDGE DECK WITH ASPHALT OVERLAY OR CONCRETE PAVEMENT WITH ASPHALT OVERLAY.
- (306) MINIMUM OF 2" OF ASPHALT.
- (307) ASPHALT ANCHOR PIN ASSEMBLY
- (308) IF DRILLING A PILOT HOLE, THE MAX. DIA. OF THE HOLE IS 3/4"
- (309) WHEN THERE IS ASPHALT OVERLAYING CONCRETE PAVEMENT, A 1 5/8" DIA. PILOT HOLE CAN BE DRILLED INTO THE OVERLAY AND CONCRETE. IF NEEDED DRILL A 3/4" PILOT HOLE IN BASE COURSE.

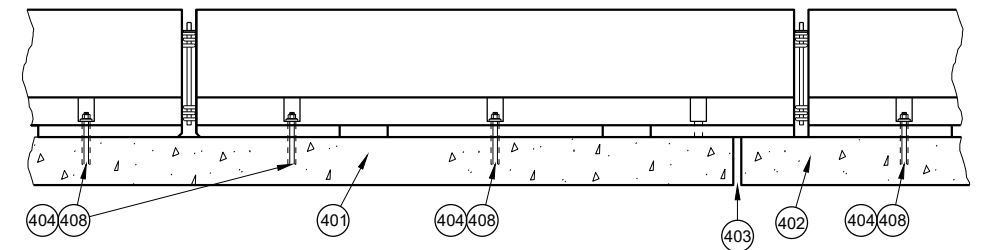


**CONCRETE BARRIER
TEMPORARY PRECAST,
12' - 6"**

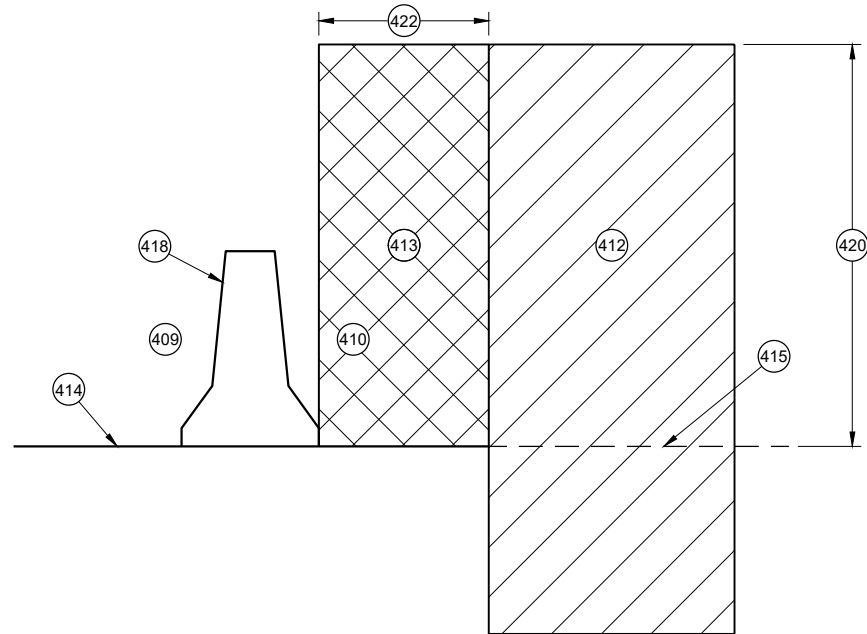
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



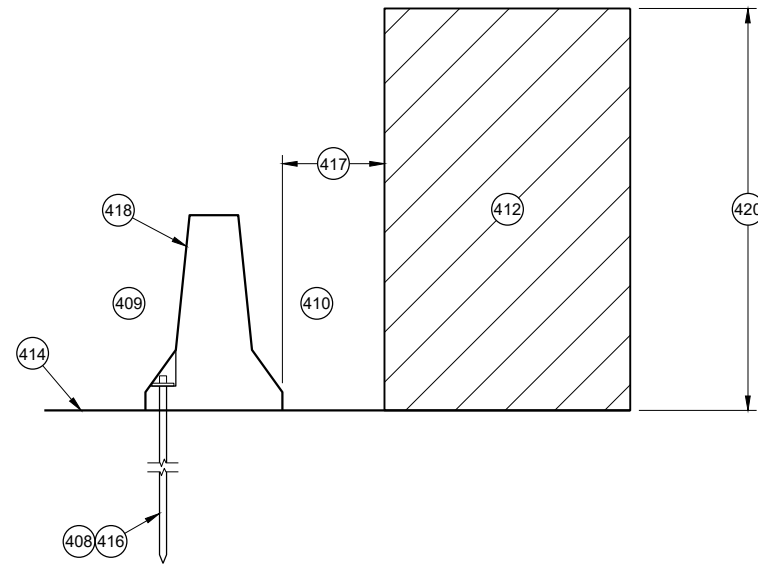
PLAN VIEW
TRANSITION FROM FREE STANDING TO ANCHORED BARRIER



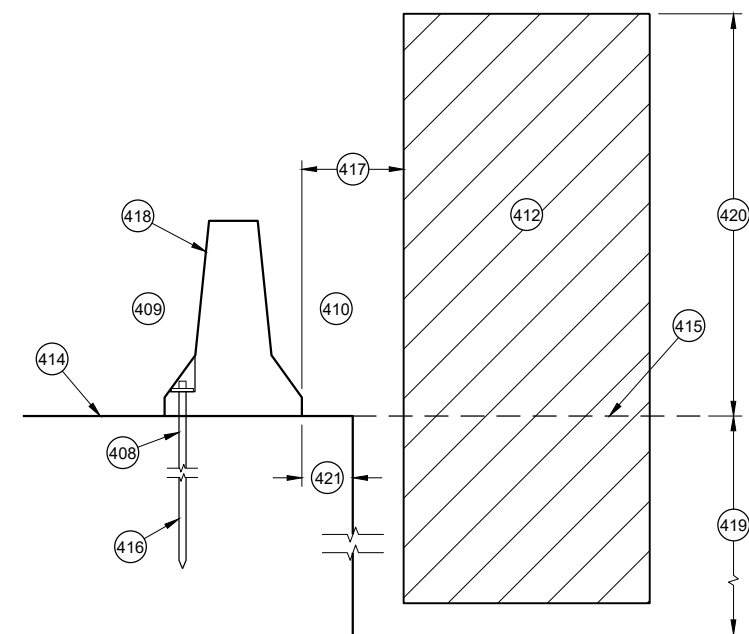
PROFILE VIEW
ANCHORED BARRIER NEAR EXPANSION JOINT



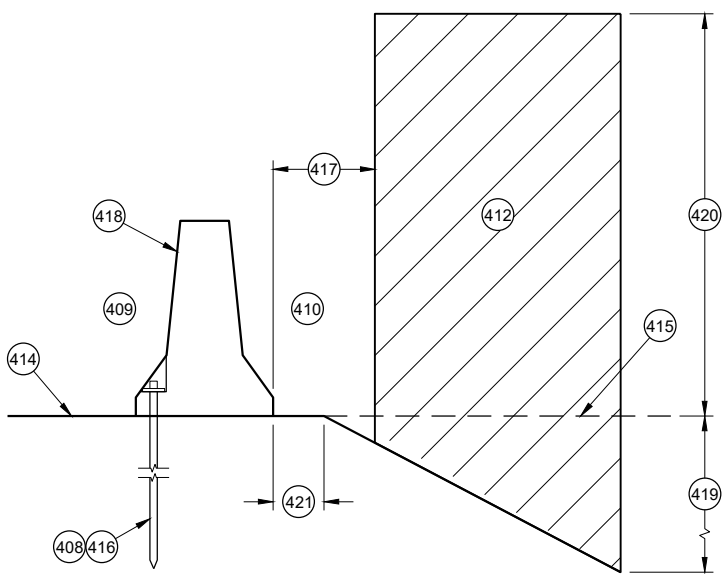
CROSS SECTION
FREE STANDING BARRIER



CROSS SECTION
ANCHORED BARRIER FOR OBJECTS ABOVE THE GRADE LINE AND NEAR THE BARRIER



CROSS SECTION
ANCHORED BARRIER NEAR VERTICAL DROP OFF



CROSS SECTION
ANCHORED BARRIER NEAR A SLOPE

GENERAL NOTES

- (400) NO SINGLE CONCRETE BARRIER SECTION SHALL BE ANCHORED TO BOTH THE BRIDGE DECK AND THE APPROACH SLAB. ALL ANCHOR BOLT LOCATIONS SHALL BE ANCHORED TO THE DECK IN ACCORDANCE WITH THE DETAIL. NO MORE THAN ONE ANCHOR BOLT SHALL BE ELIMINATED FROM A BARRIER SECTION WHEN SPANNING AN EXPANSION JOINT.
- (401) CONCRETE DECK
- (402) CONCRETE DECK OR APPROACH SLAB.
- (403) EXPANSION JOINT
- (404) ADHESIVE ANCHOR SHOWN. SEE ANCHOR DETAILS.
- (405) ANCHORED TEMPORARY BARRIER
- (406) TRANSITION FROM ANCHORED TEMPORARY BARRIER TO FREE STANDING
- (407) FREE STANDING BARRIER
- (408) REMOVE ALL ANCHORS WHEN NO LONGER NEEDED. FILL CONCRETE PAVEMENTS, DECKS AND APPROACH SLABS WITH NON-SHRINK COMMERCIAL GROUT FROM THE APPROVED PRODUCT LIST. FILL ASPHALT PAVEMENTS WITH ASTM D6690 TYPE II RUBBERIZED CRACK FILLER.
- (409) TRAFFIC SIDE
- (410) NON-TRAFFIC SIDE
- (411) ANCHOR LOCATION. SEE ANCHORING DETAILS.
- (412) WORK AREA
- (413) AREA FREE OF OBJECTS AND WORKERS
- (414) GRADE LINE
- (415) EXTENDED GRADE LINE
- (416) ANCHORED TEMPORARY BARRIER. SEE BOLT THROUGH DECK, REMOVABLE ADHESIVE ANCHOR, OR AN ASPHALT ANCHOR ROD DETAILS FOR MORE INFORMATION. ASPHALT ANCHOR ROD SHOWN.
- (417) WHEN OBJECTS EXTEND ABOVE THE GRADE. A MINIMUM OF 1 FOOT IS REQUIRED FROM BACK OF BARRIER TO OBJECT.
- (418) OBJECTS ARE NOT TO BE PLACED ON, MOUNTED TO, OR ALLOWED TO LEAN AGAINST THE BARRIER WITHOUT WRITTEN PERMISSION OF THE PROJECT ENGINEER.
- (419) DEPTHS OF 3 FEET OR MORE.
- (420) Y = 6.5'
- (421) OFFSET FROM BACK OF BARRIER EDGE:
 CONCRETE PAVEMENT 0.5'
 ASPHALT 0.5'
- (422) POSTED SPEED (MPH):
 45 OR GREATER 4.0'
 40 OR LOWER 2.0'

CONCRETE BARRIER
TEMPORARY PRECAST,
12' - 6"

STATE OF WISCONSIN
 DEPARTMENT OF TRANSPORTATION

BILL OF MATERIALS - CONCRETE BARRIER PRECAST

PART	DESCRIPTION	MATERIALS SPECIFICATIONS	NOTES
A1	PRECAST TEMPORARY BARRIER - CONCRETE	MIN. = f _c 5000 PSI	
B1	REBAR	STANDARD SPEC. 505.2 GRADE 60 UNCOATED REBAR	#5 REBAR, LENGTH 12'-2"
B2	REBAR	STANDARD SPEC. 505.2 GRADE 60 UNCOATED REBAR	#4 REBAR, LENGTH 12'-2"
B3	REBAR	STANDARD SPEC. 505.2 GRADE 60 UNCOATED REBAR	#5 REBAR, LENGTH 12'-2"
B4	REBAR	STANDARD SPEC. 505.2 GRADE 60 UNCOATED REBAR	#4 REBAR, LENGTH 6'-0"
B5	REBAR	STANDARD SPEC. 505.2 GRADE 60 UNCOATED REBAR	#6 REBAR, LENGTH 2'-11"
B6	REBAR	STANDARD SPEC. 505.2 GRADE 60 UNCOATED REBAR	#4 REBAR, LENGTH 1'-11"
B7	REBAR	STANDARD SPEC. 505.2 GRADE 60 UNCOATED REBAR	#4 REBAR, LENGTH 2'-2"
B8	REBAR	STANDARD SPEC. 505.2 GRADE 60 UNCOATED REBAR	#4 REBAR, LENGTH 2'-6"
B9	REBAR	STANDARD SPEC. 505.2 GRADE 60 UNCOATED REBAR	#4 REBAR, LENGTH 2'-9"
B10	REBAR	STANDARD SPEC. 505.2 GRADE 60 UNCOATED REBAR	#4 REBAR, LENGTH 3'-2"
B11	REBAR	STANDARD SPEC. 505.2 GRADE 60 UNCOATED REBAR	#4 REBAR, LENGTH 3'-4"
B12	REBAR	STANDARD SPEC. 505.2 GRADE 60 UNCOATED REBAR	#4 REBAR, LENGTH 12'-0"
B13	REBAR	STANDARD SPEC. 505.2 GRADE 60 UNCOATED REBAR	#4 REBAR, LENGTH 7'-9"
B14	REBAR	STANDARD SPEC. 505.2 GRADE 60 UNCOATED REBAR	#5 REBAR, LENGTH 11'-9"
C1	LOOP BAR	ASTM A709 GRADE 70 SMOOTH BAR OR ASTM A706 GRADE 60 REBAR UNCOATED	¾" DIA.
C2	LOOP BAR	ASTM A709 GRADE 70 SMOOTH BAR OR ASTM A706 GRADE 60 REBAR UNCOATED	¾" DIA.
C3	LOOP BAR	ASTM A709 GRADE 70 SMOOTH BAR OR ASTM A706 GRADE 60 REBAR UNCOATED	¾" DIA.
D1	CONNECTION PIN - ROD	ASTM A36 MIN. STRENGTH 36 KSI / ASTM A529 MAX. STRENGTH 50 KSI / ASTM A572 MAX STRENGTH 50 KSI / ASTM A709 MAX STRENGTH 50 KSI / ASTM A992 MAX STRENGTH 50 KSI	1 ½" DIA.
D2	CONNECTION PIN - TOP PLATE	ASTM A36 MIN. STRENGTH 36 KSI / ASTM A529 MAX. STRENGTH 50 KSI / ASTM A572 MAX STRENGTH 50 KSI / ASTM A709 MAX STRENGTH 50 KSI / ASTM A992 MAX STRENGTH 50 KSI	
G1	BOLT THROUGH ANCHOR - THREADED ROD	HOT DIP AASHTO M232 CLASS / ASTM A153 CLASS C / ASTM F2329 C OR MECHANICAL GALVANIZE TO AASHTO M298 CLASS 55 TYPE 2 / ASTM B695 CLASS 55 TYPE 2 A307 GRADE A OR SAE J429 GRADE 2 UNC	1 ½" DIA.
G2	BOLT THROUGH ANCHOR - WASHER, SQUARE	ASTM A36 MIN. STRENGTH 36 KSI / ASTM A529 MAX. STRENGTH 50 KSI / ASTM A572 MAX STRENGTH 50 KSI / ASTM A709 MAX STRENGTH 50 KSI / ASTM A992 MAX STRENGTH 50 KSI	
G3	BOLT THROUGH ANCHOR - NUT	HOT DIP AASHTO M232 CLASS / ASTM A153 CLASS C / ASTM F2329 C OR MECHANICAL GALVANIZE TO AASHTO M298 CLASS 55 TYPE 2 / ASTM B695 CLASS 55 UNC OVER TAP NUTS AS SPECIFIED IN AASHTO 291 / ASTM A 563 HEAVY HEX HEAD ASTM A563DH OR SAE J995 GRADE 5	
H1	ADHESIVE ANCHOR - ADHESIVE	ICC-ES-AC308 5 ¼" EMBEDMENT WITH A MIN. BOND STRENGTH OF 1,650 PSI. SEE 603.2 AND 603.3.1.2 OF THE WISCONSIN STANDARD SPECIFICATIONS FOR MORE INFORMATION ON ADHESIVE ANCHORS.	
H2	ADHESIVE ANCHOR - THREADED ROD	HOT DIP AASHTO M232 CLASS / ASTM A153 CLASS C / ASTM F2329 C OR MECHANICAL GALVANIZE TO AASHTO M298 CLASS 55 TYPE 2 / ASTM B695 CLASS 55 TYPE 2 A307 GRADE A / SAE J429 GRADE 2 UNC	1 ½" DIA.
H3	ADHESIVE ANCHOR - WASHER, SQUARE	ASTM A36 MIN. STRENGTH 36 KSI / ASTM A529 MAX. STRENGTH 50 KSI / ASTM A572 MAX STRENGTH 50 KSI / ASTM A709 MAX STRENGTH 50 KSI / ASTM A992 MAX STRENGTH 50 KSI	
H4	ADHESIVE ANCHOR - NUT	HOT DIP AASHTO M232 CLASS / ASTM A153 CLASS C / ASTM F2329 C OR MECHANICAL GALVANIZE TO AASHTO M298 CLASS 55 TYPE 2 / ASTM B695 CLASS 55 UNC OVER TAP NUTS AS SPECIFIED IN AASHTO 291 / ASTM A 563 HEAVY HEX HEAD ASTM A563DH OR SAE J995 GRADE 5	
J1	ASPHALT ANCHOR PIN - ROD	ASTM A36 MIN. STRENGTH 36 KSI / ASTM A529 MAX. STRENGTH 50 KSI / ASTM A572 MAX STRENGTH 50 KSI / ASTM A709 MAX STRENGTH 50 KSI / ASTM A992 MAX STRENGTH 50 KSI	1 ½" DIA.
J2	ASPHALT ANCHOR PIN - STOP PLATE	ASTM A36 MIN. STRENGTH 36 KSI / ASTM A529 MAX. STRENGTH 50 KSI / ASTM A572 MAX STRENGTH 50 KSI / ASTM A709 MAX STRENGTH 50 KSI / ASTM A992 MAX STRENGTH 50 KSI	
K1	THRIE BEAM RAIL	AASHTO M180 CLASS A TYPE 2 APPROVED PRODUCER	12 GAUGE
L1	THRIE BEAM RAIL - TERMINAL	AASHTO M180 CLASS A TYPE 2 APPROVED PRODUCER	12 GAUGE

PART	DESCRIPTION	MATERIALS SPECIFICATIONS	NOTES
M1	SPLICE BOLT	HOT DIP AASHTO M232 CLASS / ASTM A153 CLASS C / ASTM F2329 C OR MECHANICAL GAL. TO AASHTO M298 CLASS 55 TYPE 2 / ASTM B695 CLASS 55 TYPE 2 UNC AASHTO M180 HEAD ASTM A307 GRADE B OR SAE J429 GRADE 2 OR ASTM F1554 GRADE 36	¾" DIA.
M2	SPLICE BOLT - NUT	HOT DIP AASHTO M232 CLASS / ASTM A153 CLASS C / ASTM F2329 C OR MECHANICAL GALVANIZE TO AASHTO M298 CLASS 55 TYPE 2 / ASTM B695 CLASS 55 UNC OVER TAP NUTS AS SPECIFIED IN AASHTO 291 / ASTM A 563 AASHTO M180 RECESSED HEAVY HEX HEAD ASTM A563DH OR SAE J995 GRADE 5	
N1	THRIE BEAM RAIL TERMINAL - MECHANICAL ANCHOR	HOT DIP AASHTO M232 CLASS / ASTM A153 CLASS C / ASTM F2329 C OR MECHANICAL GALVANIZE TO AASHTO M298 CLASS 55 TYPE 2 / ASTM B695 CLASS 55 UNC OVER TAP NUTS AS SPECIFIED IN AASHTO 291 / ASTM A 563 AASHTO M180 RECESSED HEAVY HEX HEAD ASTM A563DH OR SAE J995 GRADE 5	¾" DIA. LENGTH 6"
N2	THRIE BEAM RAIL TERMINAL - WASHER	HOT DIP AASHTO M232 CLASS / ASTM A153 CLASS C / ASTM F2329 C OR MECHANICAL GALVANIZE TO AASHTO M298 CLASS 55 TYPE 2 / ASTM B695 CLASS 55 TYPE 2 F436 TYPE 1	
N3	THRIE BEAM RAIL TERMINAL MECHANICAL OR ADHESIVE ANCHOR	MINIMUM MECHANICAL OR ADHESIVE ANCHOR STRENGTH REQUIREMENTS: ULTIMATE TENSILE LOAD 17.9 KIPS AND ULTIMATE SHEAR LOAD 21.96 KIPS. SEE 603.2 AND 603.3.1.2 OF THE WISCONSIN STANDARD SPECIFICATIONS FOR MORE INFORMATION ON ADHESIVE ANCHORS.	
P1	THRIE BEAM RAIL CONNECTION 1-BOLT	HOT DIP AASHTO M232 CLASS / ASTM A153 CLASS C / ASTM F2329 C OR MECHANICAL GALVANIZE TO AASHTO M298 CLASS 55 TYPE 2 / ASTM B695 CLASS 55 UNC OVER TAP NUTS AS SPECIFIED IN AASHTO 291 / ASTM A 563 AASHTO M180 RECESSED HEAVY HEX HEAD ASTM A563DH OR SAE J995 GRADE 5	¾" DIA.
P2	THRIE BEAM RAIL CONNECTION 1-WASHER	HOT DIP AASHTO M232 CLASS / ASTM A153 CLASS C / ASTM F2329 C OR MECHANICAL GAL. TO AASHTO M298 CLASS 55 TYPE 2 / ASTM B695 CLASS 55 TYPE 2 F436 TYPE 1	
P3	THRIE BEAM RAIL CONNETION 1- MECHANICAL OR ADHESIVE ANCHOR	MINIMUM MECHANICAL OR ADHESIVE ANCHOR STRENGTH REQUIREMENTS: ULTIMATE TENSILE LOAD 9.48 KIPS AND ULTIMATE SHEAR LOAD 10.48 KIPS. SEE 603.2 AND 603.3.1.2 OF THE WISCONSIN STANDARD SPECIFICATIONS FOR MORE INFORMATION ON ADHESIVE ANCHORS.	
Q1	BLOCK WOOD	SEE STANDARD SPEC. 614	
R1	CAP - BOLT	HOT DIP AASHTO M232 CLASS / ASTM A153 CLASS C / ASTM F2329 C OR MECHANICAL GALVANIZE TO AASHTO M298 CLASS 55 TYPE 2 / ASTM B695 CLASS 55 UNC OVER TAP NUTS AS SPECIFIED IN AASHTO 291 / ASTM A 563 AASHTO M180 RECESSED HEAVY HEX HEAD ASTM A563DH OR SAE J995 GRADE 5	¾" DIA.
R2	CAP - BOLT - WASHER	HOT DIP AASHTO M232 CLASS / ASTM A153 CLASS C / ASTM F2329 C OR MECHANICAL GAL. TO AASHTO M298 CLASS 55 TYPE 2 / ASTM B695 CLASS 55 TYPE 2 F436 TYPE 1	
R3	CAP - BOLT - MECHANICAL ANCHOR	MINIMUM MECHANICAL OR ADHESIVE ANCHOR STRENGTH REQUIREMENTS ULTIMATE TENSILE LOAD 12.14 KIPS AND ULTIMATE SHEAR LOAD 17.5 KIPS. SEE 603.2 AND 603.3.1.2 OF THE WISCONSIN STANDARD SPECIFICATIONS FOR MORE INFORMATION ON ADHESIVE ANCHORS.	12 GAUGE
S1	CAP 42-INCH TOP PLATE	AASHTO M111 / ASTM A123 ASTM A36 MIN. STRENGTH 36 KSI, OR ASTM A529 MAX. STRENGTH 50 KSI, OR ASTM A572 MAX. STRENGTH 50 KSI, OR ASTM A709 MAX. STRENGTH 50 KSI, OR ASTM A992 MAX. STRENGTH 50 KSI	12 GAUGE
S2	CAP 42-INCH END PLATE	AASHTO M111 / ASTM A123 ASTM A36 MIN. STRENGTH 36 KSI, OR ASTM A529 MAX. STRENGTH 50 KSI, OR ASTM A572 MAX. STRENGTH 50 KSI, OR ASTM A709 MAX. STRENGTH 50 KSI, OR ASTM A992 MAX. STRENGTH 50 KSI	12 GAUGE
S3	CAP 42-INCH SIDE PLATE	AASHTO M111 / ASTM A123 ASTM A36 MIN. STRENGTH 36 KSI, OR ASTM A529 MAX. STRENGTH 50 KSI, OR ASTM A572 MAX. STRENGTH 50 KSI, OR ASTM A709 MAX. STRENGTH 50 KSI, OR ASTM A992 MAX. STRENGTH 50 KSI	12 GAUGE
S4	CAP 42-INCH GUSSET 1	AASHTO M111 / ASTM A123 ASTM A36 MIN. STRENGTH 36 KSI, OR ASTM A529 MAX. STRENGTH 50 KSI, OR ASTM A572 MAX. STRENGTH 50 KSI, OR ASTM A709 MAX. STRENGTH 50 KSI, OR ASTM A992 MAX. STRENGTH 50 KSI	12 GAUGE
S5	CAP 42-INCH GUSSET 2	AASHTO M111 / ASTM A123 ASTM A36 MIN. STRENGTH 36 KSI, OR ASTM A529 MAX. STRENGTH 50 KSI, OR ASTM A572 MAX. STRENGTH 50 KSI, OR ASTM A709 MAX. STRENGTH 50 KSI, OR ASTM A992 MAX. STRENGTH 50 KSI	12 GAUGE
S6	CAP 42-INCH GUSSET 3	AASHTO M111 / ASTM A123 ASTM A36 MIN. STRENGTH 36 KSI, OR ASTM A529 MAX. STRENGTH 50 KSI, OR ASTM A572 MAX. STRENGTH 50 KSI, OR ASTM A709 MAX. STRENGTH 50 KSI, OR ASTM A992 MAX. STRENGTH 50 KSI	12 GAUGE
S7	CAP 42-INCH GUSSET 4	AASHTO M111 / ASTM A123 ASTM A36 MIN. STRENGTH 36 KSI, OR ASTM A529 MAX. STRENGTH 50 KSI, OR ASTM A572 MAX. STRENGTH 50 KSI, OR ASTM A709 MAX. STRENGTH 50 KSI, OR ASTM A992 MAX. STRENGTH 50 KSI	12 GAUGE

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SDD 14B07-16m

6

SDD 14B07-16m

**CONCRETE BARRIER
TEMPORARY PRECAST,
12' - 6"**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

BILL OF MATERIALS - CONCRETE BARRIER PRECAST

PART	DESCRIPTION	MATERIALS SPECIFICATIONS	NOTES
T1	CAP 56-INCH TOP PLATE	AASHTO M111 / ASTM A123 ASTM A36 MIN. STRENGTH 36 KSI, OR ASTM A529 MAX. STRENGTH 50 KSI, OR ASTM A572 MAX. STRENGTH 50 KSI, OR ASTM A709 MAX. STRENGTH 50 KSI, OR ASTM A992 MAX. STRENGTH 50 KSI	12 GAUGE
T2	CAP 56-INCH END PLATE	AASHTO M111 / ASTM A123 ASTM A36 MIN. STRENGTH 36 KSI, OR ASTM A529 MAX. STRENGTH 50 KSI, OR ASTM A572 MAX. STRENGTH 50 KSI, OR ASTM A709 MAX. STRENGTH 50 KSI, OR ASTM A992 MAX. STRENGTH 50 KSI	12 GAUGE
T3	CAP 56-INCH SIDE PLATE 1	AASHTO M111 / ASTM A123 ASTM A36 MIN. STRENGTH 36 KSI, OR ASTM A529 MAX. STRENGTH 50 KSI, OR ASTM A572 MAX. STRENGTH 50 KSI, OR ASTM A709 MAX. STRENGTH 50 KSI, OR ASTM A992 MAX. STRENGTH 50 KSI	12 GAUGE
T4	CAP 56-INCH SIDE PLATE 2	AASHTO M111 / ASTM A123 ASTM A36 MIN. STRENGTH 36 KSI, OR ASTM A529 MAX. STRENGTH 50 KSI, OR ASTM A572 MAX. STRENGTH 50 KSI, OR ASTM A709 MAX. STRENGTH 50 KSI, OR ASTM A992 MAX. STRENGTH 50 KSI	12 GAUGE
T5	CAP 56-INCH GUSSET 1	AASHTO M111 / ASTM A123 ASTM A36 MIN. STRENGTH 36 KSI, OR ASTM A529 MAX. STRENGTH 50 KSI, OR ASTM A572 MAX. STRENGTH 50 KSI, OR ASTM A709 MAX. STRENGTH 50 KSI, OR ASTM A992 MAX. STRENGTH 50 KSI	12 GAUGE
T6	CAP 56-INCH GUSSET 2	AASHTO M111 / ASTM A123 ASTM A36 MIN. STRENGTH 36 KSI, OR ASTM A529 MAX. STRENGTH 50 KSI, OR ASTM A572 MAX. STRENGTH 50 KSI, OR ASTM A709 MAX. STRENGTH 50 KSI, OR ASTM A992 MAX. STRENGTH 50 KSI	12 GAUGE
T7	CAP 56-INCH GUSSET 3	AASHTO M111 / ASTM A123 ASTM A36 MIN. STRENGTH 36 KSI, OR ASTM A529 MAX. STRENGTH 50 KSI, OR ASTM A572 MAX. STRENGTH 50 KSI, OR ASTM A709 MAX. STRENGTH 50 KSI, OR ASTM A992 MAX. STRENGTH 50 KSI	12 GAUGE
T8	CAP 42-INCH GUSSET 4	AASHTO M111 / ASTM A123 ASTM A36 MIN. STRENGTH 36 KSI, OR ASTM A529 MAX. STRENGTH 50 KSI, OR ASTM A572 MAX. STRENGTH 50 KSI, OR ASTM A709 MAX. STRENGTH 50 KSI, OR ASTM A992 MAX. STRENGTH 50 KSI	12 GAUGE
T9	CAP 42-INCH GUSSET 5	AASHTO M111 / ASTM A123 ASTM A36 MIN. STRENGTH 36 KSI, OR ASTM A529 MAX. STRENGTH 50 KSI, OR ASTM A572 MAX. STRENGTH 50 KSI, OR ASTM A709 MAX. STRENGTH 50 KSI, OR ASTM A992 MAX. STRENGTH 50 KSI	12 GAUGE
T10	CAP 42-INCH GUSSET 6	AASHTO M111 / ASTM A123 ASTM A36 MIN. STRENGTH 36 KSI, OR ASTM A529 MAX. STRENGTH 50 KSI, OR ASTM A572 MAX. STRENGTH 50 KSI, OR ASTM A709 MAX. STRENGTH 50 KSI, OR ASTM A992 MAX. STRENGTH 50 KSI	12 GAUGE
T11	CAP 42-INCH GUSSET 7	AASHTO M111 / ASTM A123 ASTM A36 MIN. STRENGTH 36 KSI, OR ASTM A529 MAX. STRENGTH 50 KSI, OR ASTM A572 MAX. STRENGTH 50 KSI, OR ASTM A709 MAX. STRENGTH 50 KSI, OR ASTM A992 MAX. STRENGTH 50 KSI	12 GAUGE
T12	CAP 42-INCH GUSSET 8	AASHTO M111 / ASTM A123 ASTM A36 MIN. STRENGTH 36 KSI, OR ASTM A529 MAX. STRENGTH 50 KSI, OR ASTM A572 MAX. STRENGTH 50 KSI, OR ASTM A709 MAX. STRENGTH 50 KSI, OR ASTM A992 MAX. STRENGTH 50 KSI	12 GAUGE
T13	CAP 42-INCH GUSSET 9	AASHTO M111 / ASTM A123 ASTM A36 MIN. STRENGTH 36 KSI, OR ASTM A529 MAX. STRENGTH 50 KSI, OR ASTM A572 MAX. STRENGTH 50 KSI, OR ASTM A709 MAX. STRENGTH 50 KSI, OR ASTM A992 MAX. STRENGTH 50 KSI	12 GAUGE
T14	CAP 42-INCH GUSSET 10	AASHTO M111 / ASTM A123 ASTM A36 MIN. STRENGTH 36 KSI, OR ASTM A529 MAX. STRENGTH 50 KSI, OR ASTM A572 MAX. STRENGTH 50 KSI, OR ASTM A709 MAX. STRENGTH 50 KSI, OR ASTM A992 MAX. STRENGTH 50 KSI	12 GAUGE
T15	CAP 42-INCH GUSSET 11	AASHTO M111 / ASTM A123 ASTM A36 MIN. STRENGTH 36 KSI, OR ASTM A529 MAX. STRENGTH 50 KSI, OR ASTM A572 MAX. STRENGTH 50 KSI, OR ASTM A709 MAX. STRENGTH 50 KSI, OR ASTM A992 MAX. STRENGTH 50 KSI	12 GAUGE
T16	CAP 42-INCH GUSSET 12	AASHTO M111 / ASTM A123 ASTM A36 MIN. STRENGTH 36 KSI, OR ASTM A529 MAX. STRENGTH 50 KSI, OR ASTM A572 MAX. STRENGTH 50 KSI, OR ASTM A709 MAX. STRENGTH 50 KSI, OR ASTM A992 MAX. STRENGTH 50 KSI	12 GAUGE
U1	GAP STIFFENER	AASHTO M111 / ASTM A123 ASTM A36 MIN. STRENGTH 36 KSI, OR ASTM A529 MAX. STRENGTH 50 KSI, OR ASTM A572 MAX. STRENGTH 50 KSI, OR ASTM A709 MAX. STRENGTH 50 KSI, OR ASTM A992 MAX. STRENGTH 50 KSI	
U2	GAP STIFFENER - CONNECTOR PLATE 1	AASHTO M111 / ASTM A123 ASTM A36 MIN. STRENGTH 36 KSI, OR ASTM A529 MAX. STRENGTH 50 KSI, OR ASTM A572 MAX. STRENGTH 50 KSI, OR ASTM A709 MAX. STRENGTH 50 KSI, OR ASTM A992 MAX. STRENGTH 50 KSI	
U3	GAP STIFFENER - CONNECTOR PLATE 2	AASHTO M111 / ASTM A123 ASTM A36 MIN. STRENGTH 36 KSI, OR ASTM A529 MAX. STRENGTH 50 KSI, OR ASTM A572 MAX. STRENGTH 50 KSI, OR ASTM A709 MAX. STRENGTH 50 KSI, OR ASTM A992 MAX. STRENGTH 50 KSI	

PART	DESCRIPTION	MATERIALS SPECIFICATIONS	NOTES
V1	THRIE BEAM RAIL TERMINAL MECHANICAL OR ADHESIVE ANCHOR	MINIMUM MECHANICAL OR ADHESIVE ANCHOR STRENGTH REQUIREMENTS ULTIMATE TENSILE LOAD 24.0 KIPS AND ULTIMATE SHEAR LOAD 21.5 KIPS. SEE 603.2 AND 603.3.1.2 OF THE WISCONSIN STANDARD SPECIFICATIONS FOR MORE INFORMATION ON ADHESIVE ANCHORS.	¾" DIA.
V2	GAP STIFFENER - BOLT - NUT	HOT DIP AASHTO M232 CLASS / ASTM A153 CLASS C / ASTM F2329 C O R MECHANICAL GALVANIZE TO AASHTO M298 CLASS 50 TYPE 1 / ASTM B695 CLASS 50 TYPE 1 UNC OVER TAP NUTS AS SPECIFIED IN AASHTO 291/ASTM A 563 HEAVY HEX HEAD ASTM A563DH OR SAE J995 GRADE 5	
W1	TOE PLATE	AASHTO M111/ASTM A123 ASTM A36 MIN STRENGTH 36 KSI, OR ASTM A529 MAX. STRENGTH 50 KSI, OR ASTM A572 MAX. STRENGTH 50 KSI, OR ASTM A709 MAX. STRENGTH 50 KSI, OR ASTM A992 MAX. STRENGTH 50 KSI	
X1	TOE PLATE - CONNECTION BOLT	HOT DIP AASHTO M232 CLASS / ASTM A153 CLASS C / ASTM F2329 C OR MECHANICAL GALVANIZE TO AASHTO M298 CLASS 55 TYPE 2 / ASTM B695 CLASS 55 TYPE 2 UNC HEAVY HEX HEAD OR AASTHO M180 HEAD, ASTM F3125 GRADE A325 TYPE 1 HEAVY HEX HEAD OR SAE J429 GRADE 5 HEAVY HEX HEAD / ASTM A449 TYPE 1 HEAVY HEX HEAD. BOLTS MAY BE FULLY THREADED. PROVIDE ENOUGH THREADING FOR PROPER TIGHTENING OF BOLT.	¾" DIA.
X2	TOE PLATE - CONNECTION BOLT - WASHER	HOT DIP AASHTO M232 CLASS / ASTM A153 CLASS C / ASTM F2329 C OR MECHANICAL GALVANIZE TO AASHTO M298 CLASS 55 TYPE 2 / ASTM B695 CLASS 55 TYPE 2 F436 TYPE 1 (HARDEN WASHER ONLY)	
X3	TOE PLATE - CONNECTION BOLT - NUT	HOT DIP AASHTO M232 CLASS / ASTM A153 CLASS C / ASTM F2329 C OR MECHANICAL GALVANIZE TO AASHTO M298 CLASS 55 TYPE 2 / ASTM B695 CLASS 55 UNC OVER TAP NUTS AS SPECIFIED IN AASHTO 291 / ASTM A 563 HEAVY HEX HEAD ASTM A563DH OR SAE J995 GRADE 5	

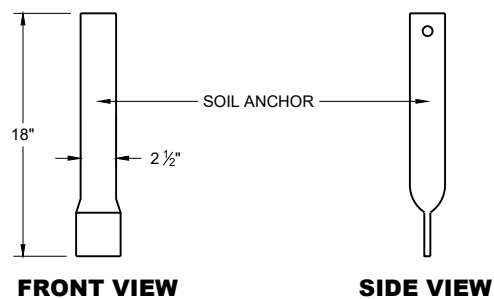
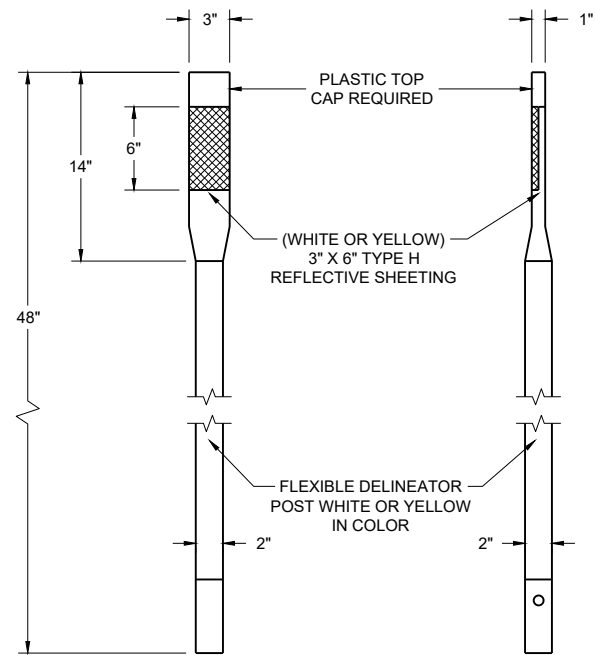
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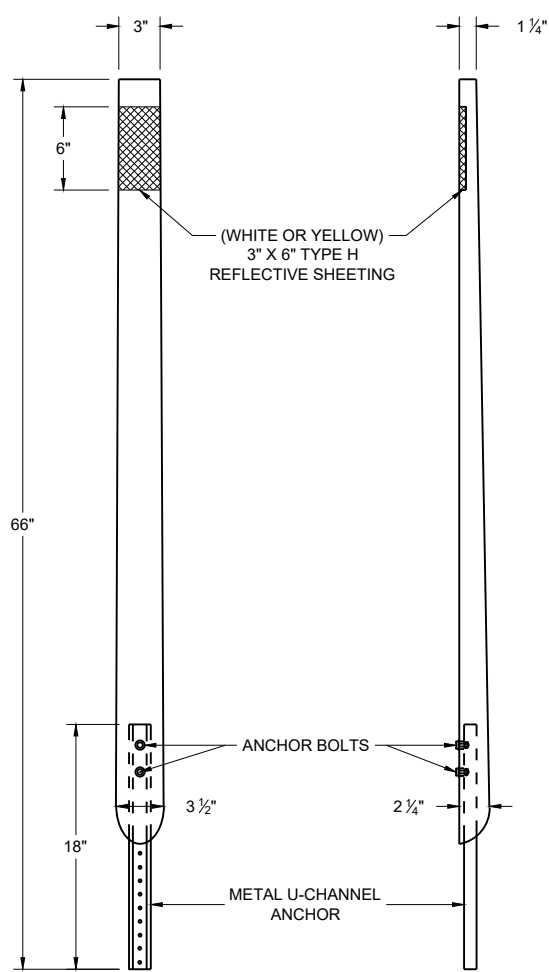
SDD 14B07-16n

SDD 14B07-16n

CONCRETE BARRIER TEMPORARY PRECAST, 12' - 6"	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED February 2023 DATE	/S/ Rodney Taylor ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA	

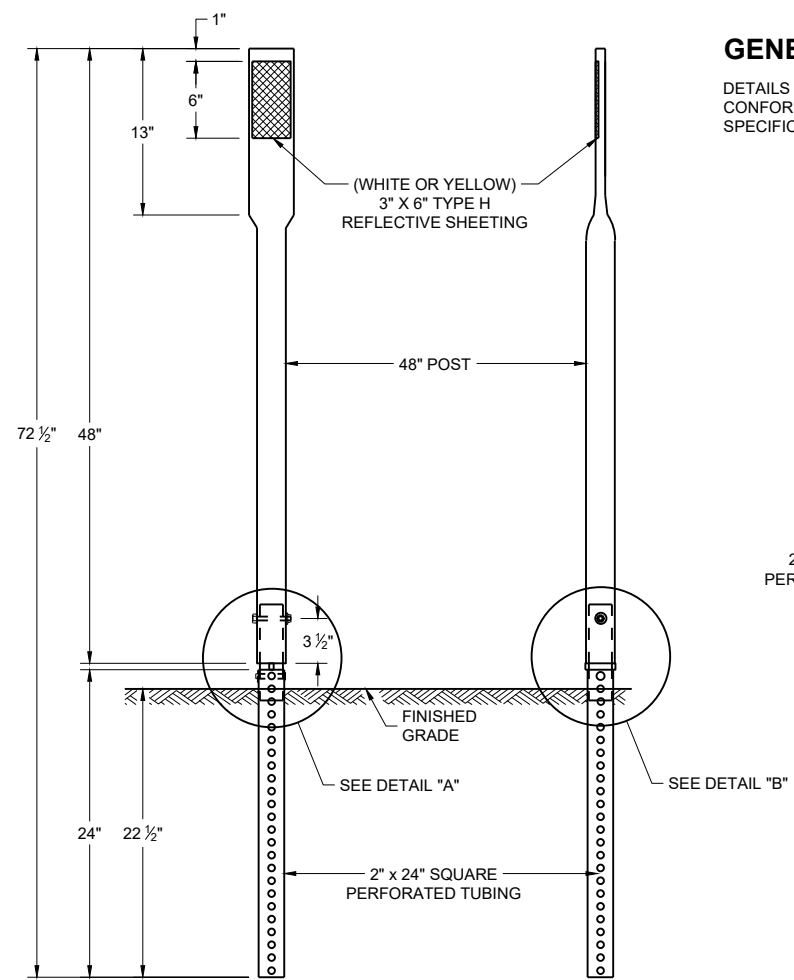


FRONT VIEW SIDE VIEW
ALTERNATE 1



FRONT VIEW SIDE VIEW
ALTERNATE 2

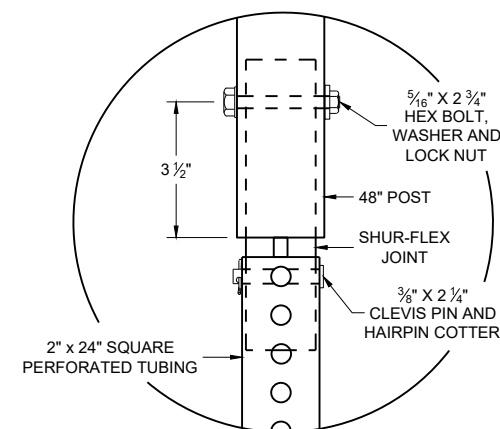
FLEXIBLE DELINEATOR POSTS



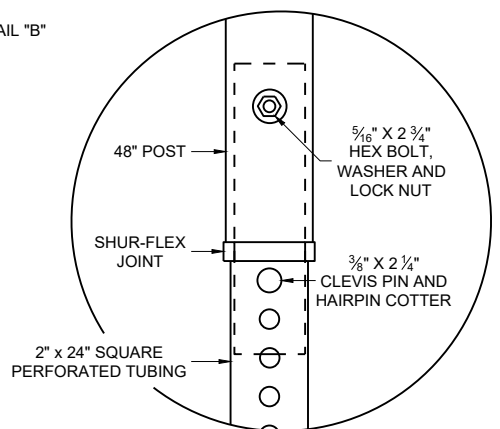
FRONT VIEW SIDE VIEW
ALTERNATE 3

GENERAL NOTES

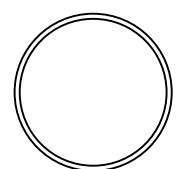
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.



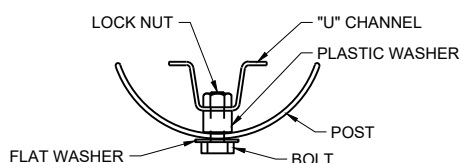
DETAIL A



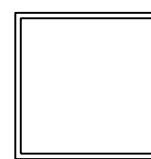
DETAIL B



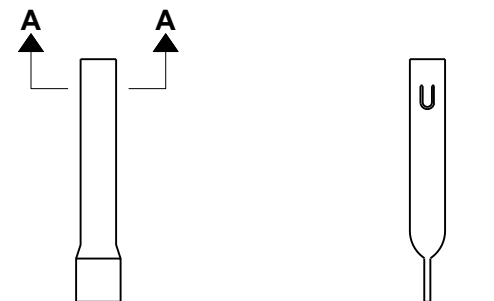
SECTION A - A



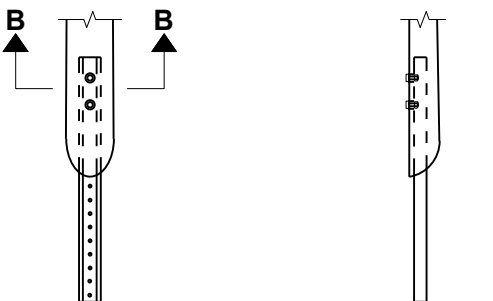
SECTION B - B



SECTION C - C

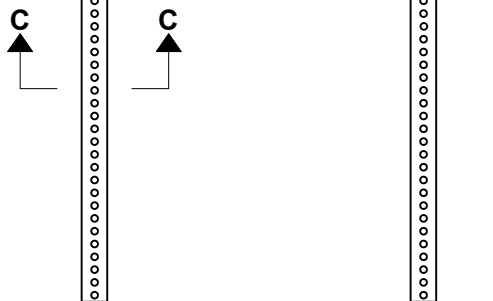


FRONT VIEW SIDE VIEW
ALTERNATE 1



FRONT VIEW SIDE VIEW
ALTERNATE 2

FLEXIBLE MARKER POST ANCHORS



FRONT VIEW SIDE VIEW
ALTERNATE 3

REFLECTOR SPACING TABLE

REFLECTOR SPACING	LOCATION
* 100' C-C	RAMPS
400' C-C	MAINLINE

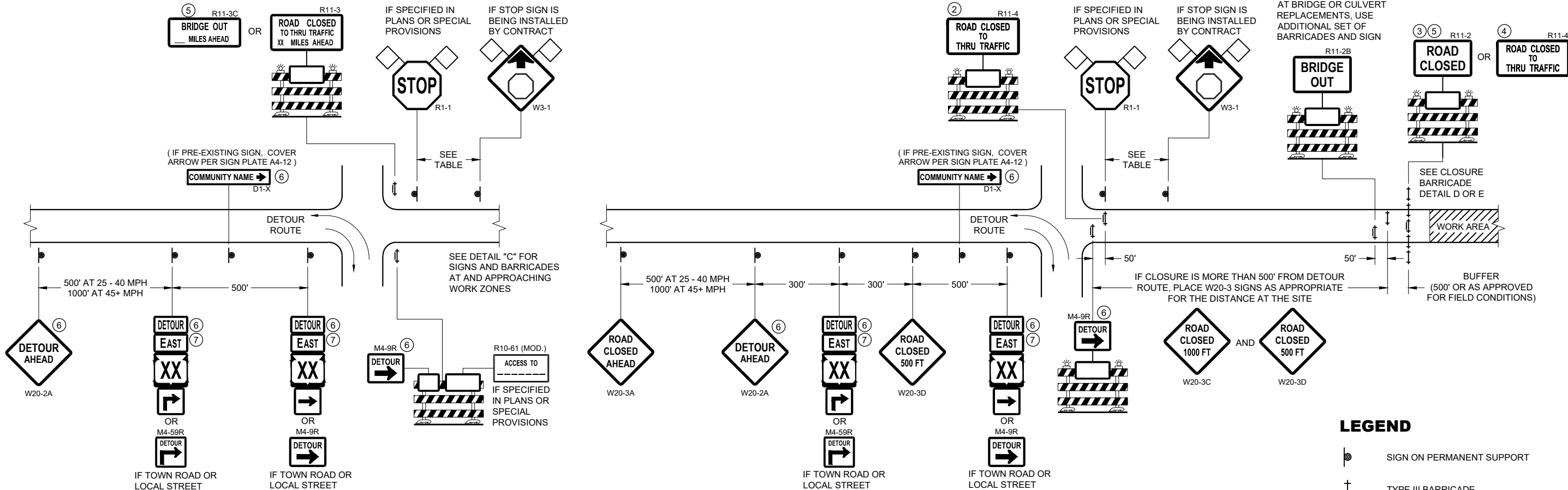
* START AT BEGINNING OF RAMP TAPER AND END AT END OF RAMP TAPER

FLEXIBLE DELINEATOR POST

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
March 2024 /S/ Jeannie Silver
DATE Statewide Pavement Marking Engineer

FHWA



**DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR**

WORK ZONE GREATER THAN OR EQUAL TO 1/2 MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)

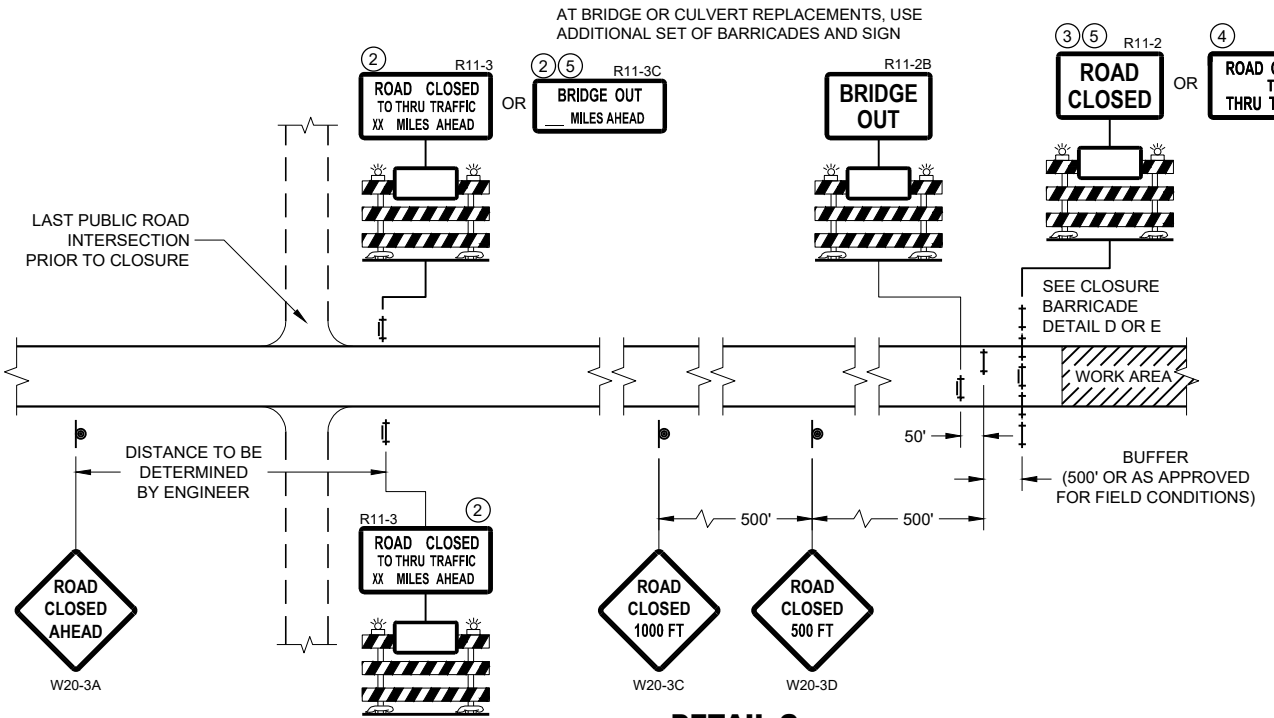
**DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR**

WORK ZONE LESS THAN 1/2 MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)

- LEGEND**
- SIGN ON PERMANENT SUPPORT
 - TYPE III BARRICADE
 - TYPE III BARRICADE WITH ATTACHED SIGN
 - TYPE "A" WARNING LIGHT (FLASHING)
 - WORK AREA
 - FLAGS, 16" X 16" MIN. (ORANGE)

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

- M4 - 8
- M3 - X
- OR OR M1 - 4 M1 - 6 M1 - 5A
- OR M05 - 1 M06 - 1



**DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR**

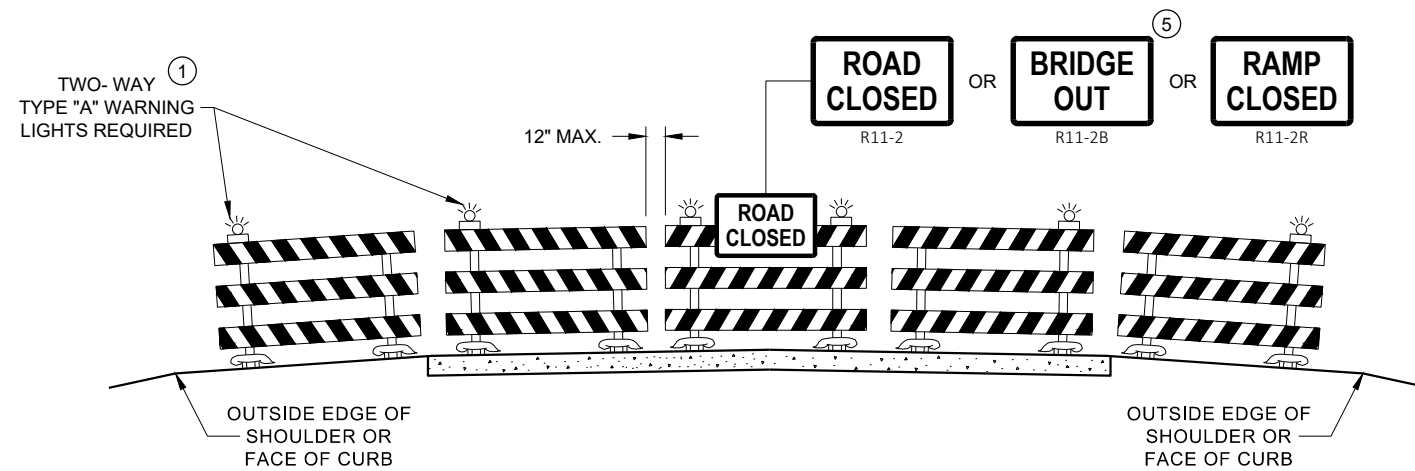
SEE SDD 15C2-SHEET "b"
FOR GENERAL NOTES
AND FOOTNOTES ① THROUGH ⑦

**BARRICADES AND SIGNS
FOR MAINLINE CLOSURES**

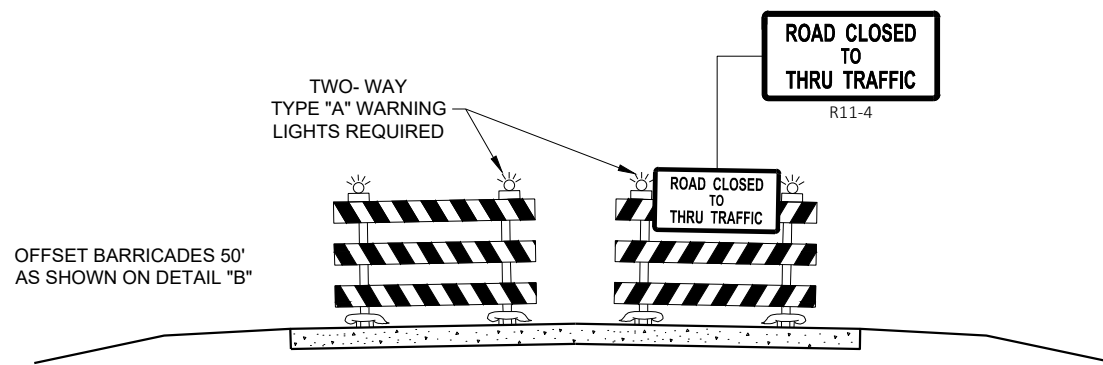
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 /S/ Andrew Heidtke
DATE DATE WORK ZONE ENGINEER

FHWA



**DETAIL D
ROAD CLOSURE BARRICADE DETAIL
APPROACH VIEW**



**DETAIL E
LANE CLOSURE BARRICADE DETAIL
APPROACH VIEW**

SEE SDD 15C2 - SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW - INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11 - 2, R11 - 3, M4 - 9, R11 - 4, AND R10 - 61 SIGNS PLACED ON THE BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE RAIL OR BOTTOM RAILS.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

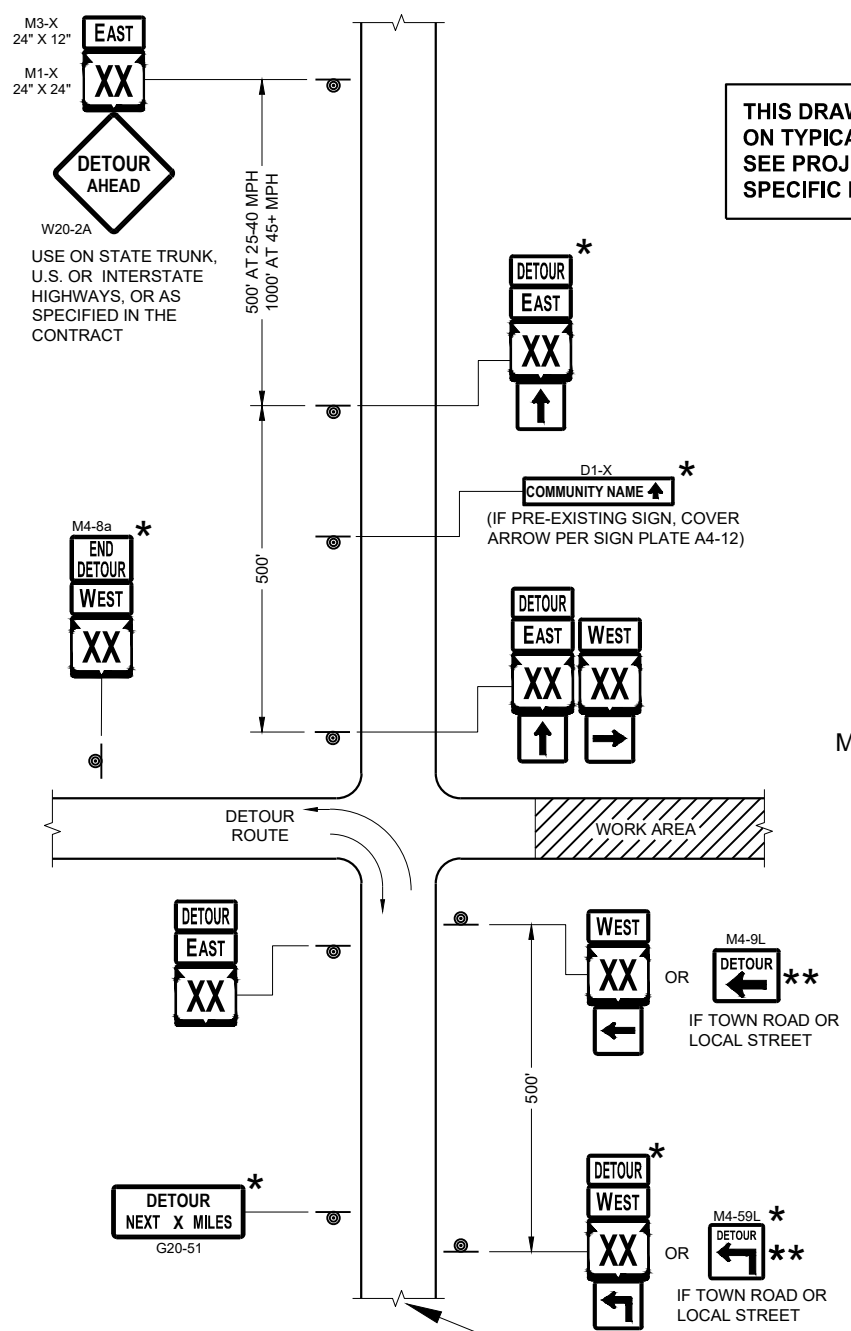
- R11 - 2 SHALL BE 48" X 30"
- R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60" X 30"
- M4 - 9 SHALL BE 30" X 24"
- M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1 - 1 SHALL BE 36" X 36"

- ① TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT SPACING).
- ② THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.
- ③ FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".
- ④ FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".
- ⑤ FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.
- ⑥ INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.
- ⑦ "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

**BARRICADES AND SIGNS
FOR
VARIOUS CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER



THIS DRAWING PROVIDES GENERAL GUIDANCE ON TYPICAL DETOUR SIGN LAYOUT AND SPACING. SEE PROJECT DETOUR SIGNING SHEETS FOR SPECIFIC DETAILS FOR EACH PROJECT.

LEGEND

- SIGN ON PERMANENT SUPPORT
- WORK AREA
- M4 - 8
- M3 - X
- M1 - 4
- M1 - 6
- M1 - 5A
- M05 - 1
- M06 - 1
- M06 - 1

GENERAL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. MODIFY EXISTING SIGNS WHERE POSSIBLE.

THE SPACING BETWEEN TRAFFIC CONTROL AND DETOUR SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

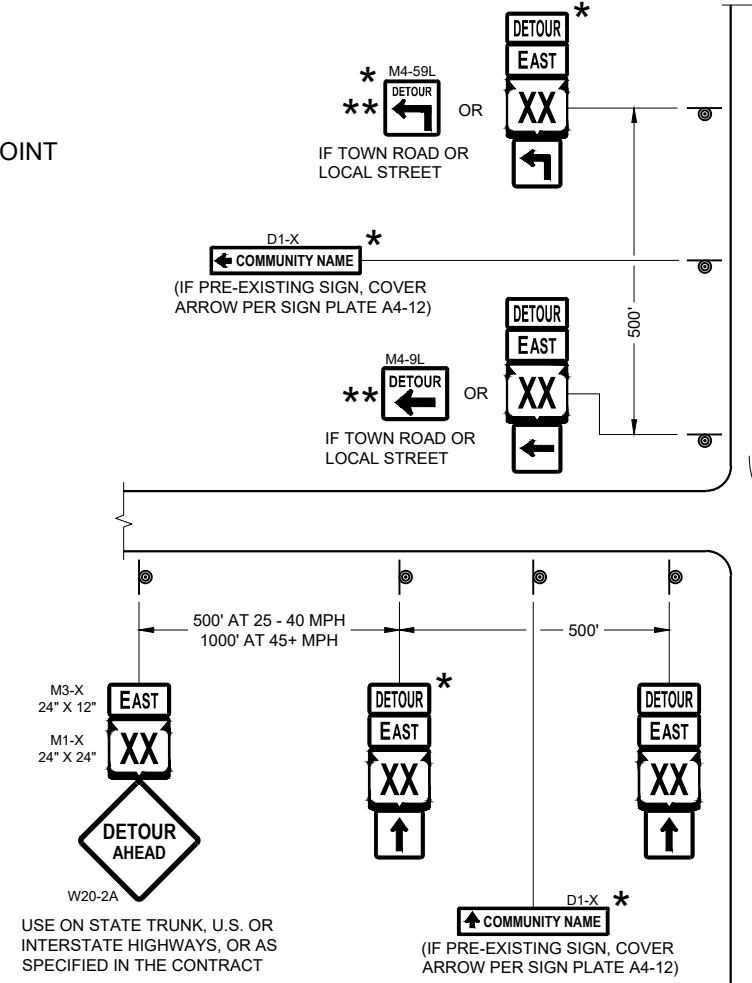
"MO" SIGNS ARE THE SAME AS "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGN SIZES SHALL BE AS FOLLOWS:

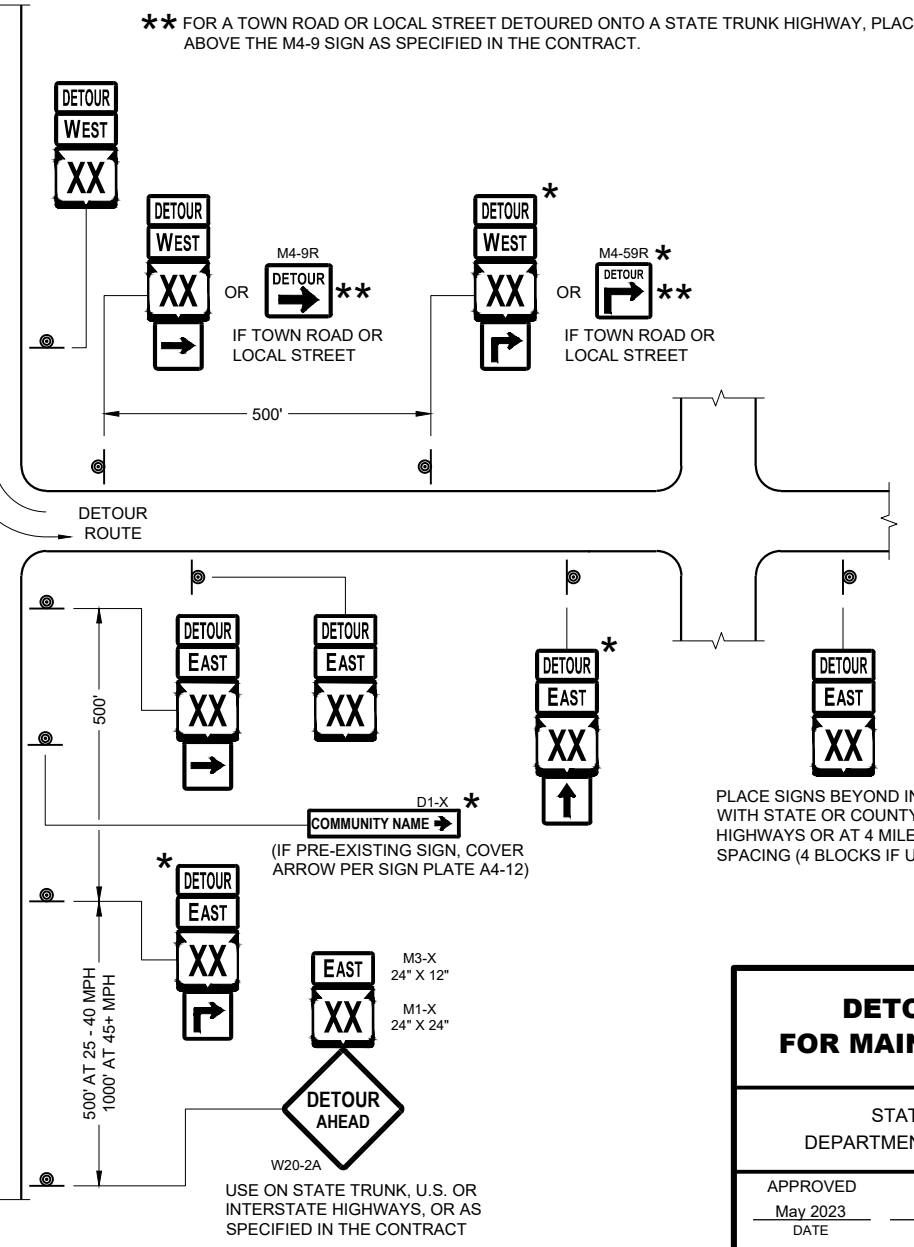
- M3-X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4-8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1-4, M1-5A AND M1-6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- M05-1 AND M06-1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- M4-9 AND M4-9R SHALL BE 30" X 24"
- M4-8a SHALL BE 24" X 18"
- G20-51 SHALL BE 60" X 24"
- W20-2A SHALL BE 48" X 48"
- D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.

- * OPTIONAL SIGNS. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS.
- ** FOR A TOWN ROAD OR LOCAL STREET DETOURED ONTO A STATE TRUNK HIGHWAY, PLACE A ROAD NAME PLAQUE ABOVE THE M4-9 SIGN AS SPECIFIED IN THE CONTRACT.

MATCH POINT



**DETAIL F
DETOUR SIGNING**



DETOUR SIGNING FOR MAINLINE CLOSURES	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2023 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.




ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS, 36"X36" SIGNS MAY BE USED INSTEAD OF 48" X 48" SIGNS.

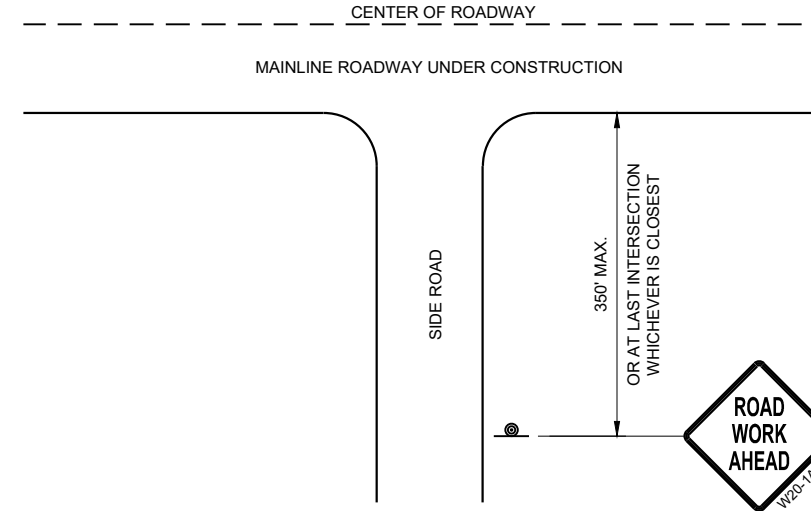
SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

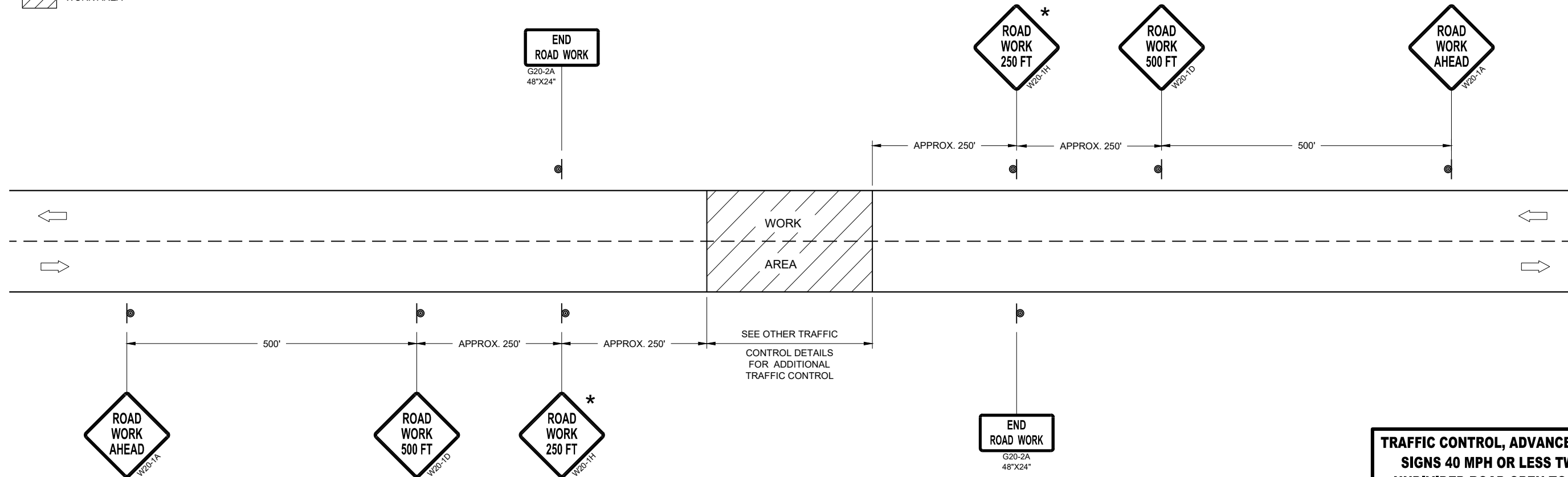
* THE THIRD W20-1 SIGN IS REQUIRED ONLY IF THERE IS AN INTERSECTION BETWEEN THE "ROAD WORK 500 FEET" SIGN AND THE WORK ZONE. ADJUST THE PLACEMENT OF THIS SIGN BASED ON INTERSECTION LOCATION AND OTHER FIELD CONDITIONS.

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC
-  WORK AREA



**TYPICAL SIDE ROAD APPROACH
WARNING SIGN DETAIL**



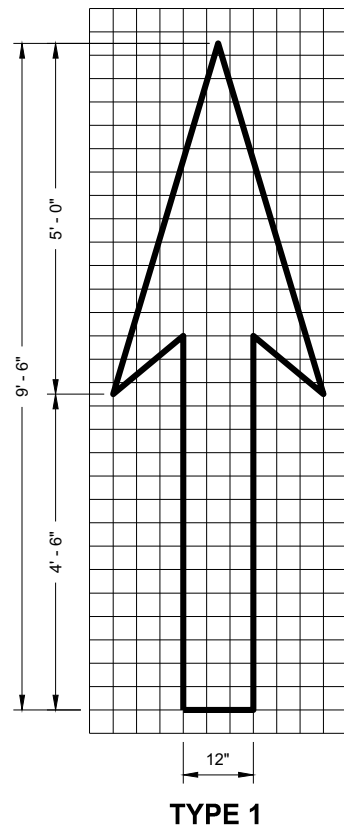
TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40MPH OR LESS

**TRAFFIC CONTROL, ADVANCE WARNING
SIGNS 40 MPH OR LESS TWO-WAY
UNDIVIDED ROAD OPEN TO TRAFFIC**

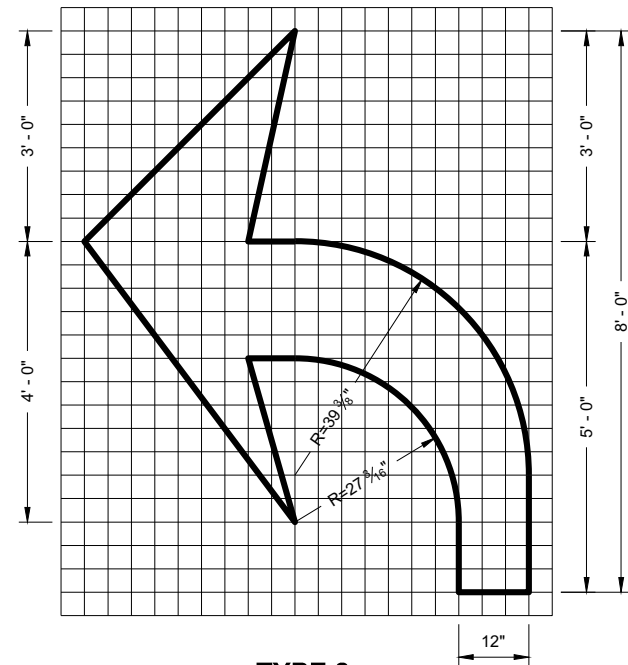
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE July 2018 /S/ Andrew Heidtke
WORK ZONE ENGINEER

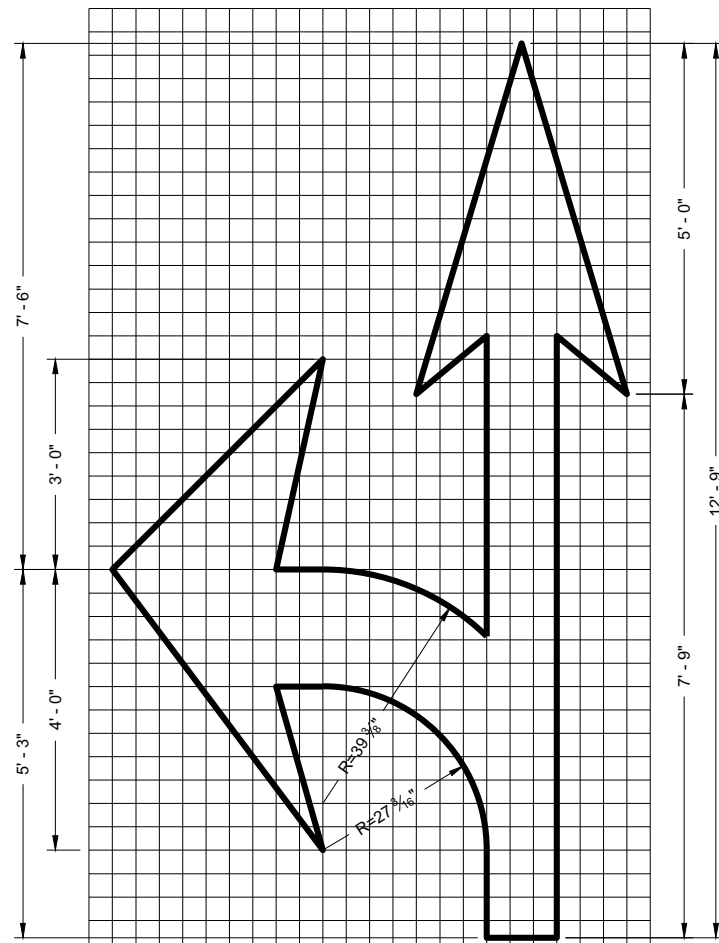
FHWA



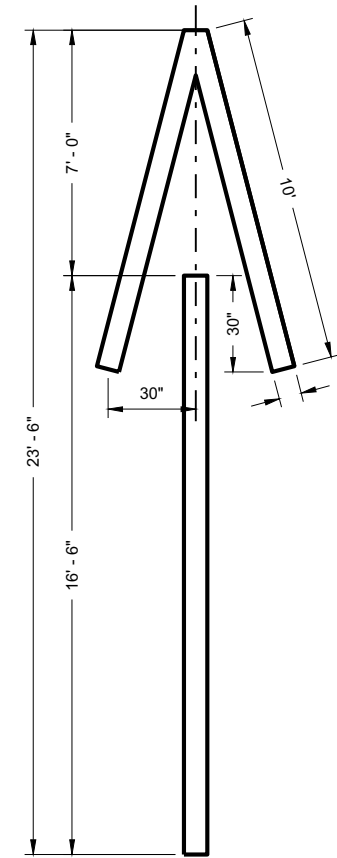
TYPE 1



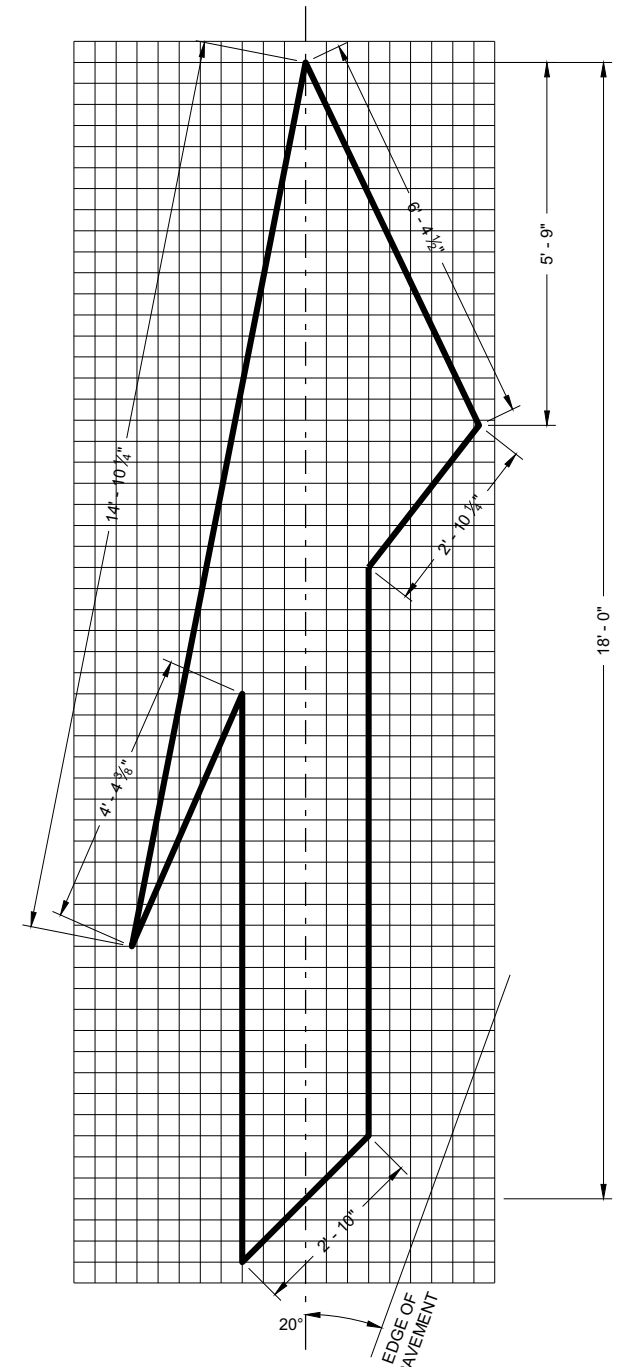
TYPE 2



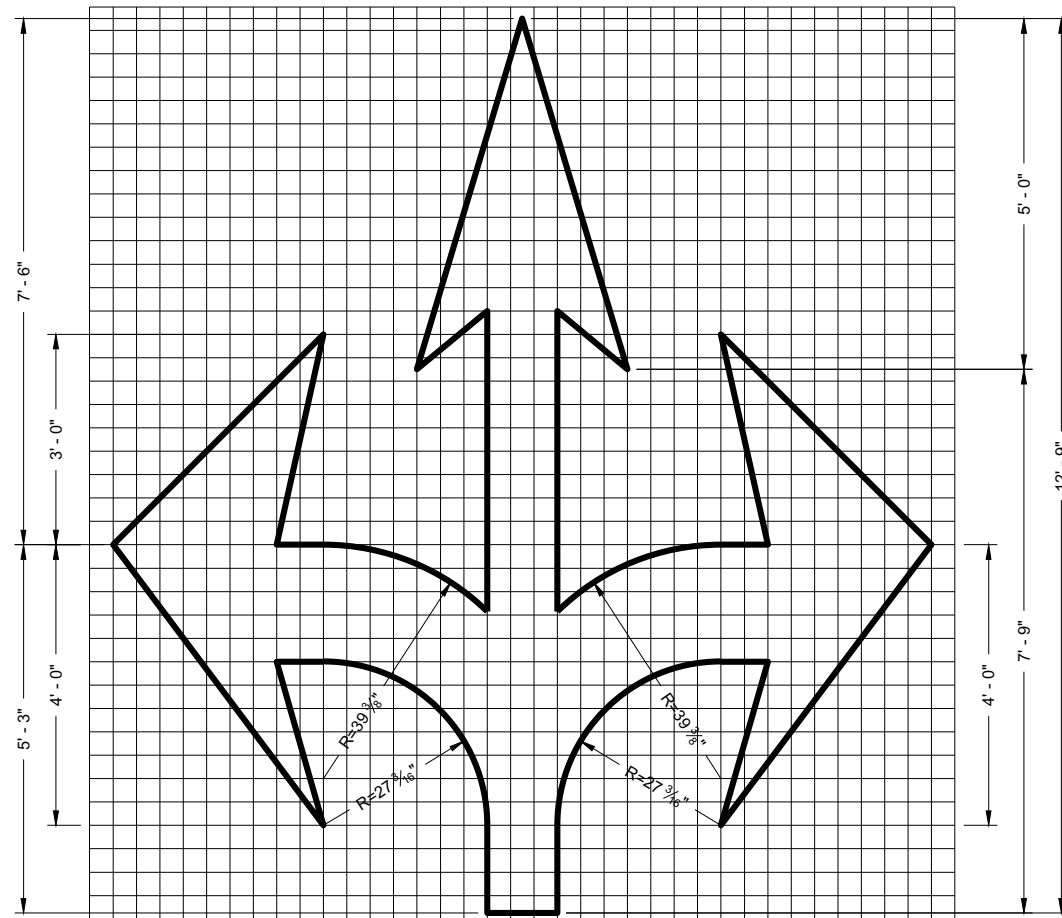
TYPE 3



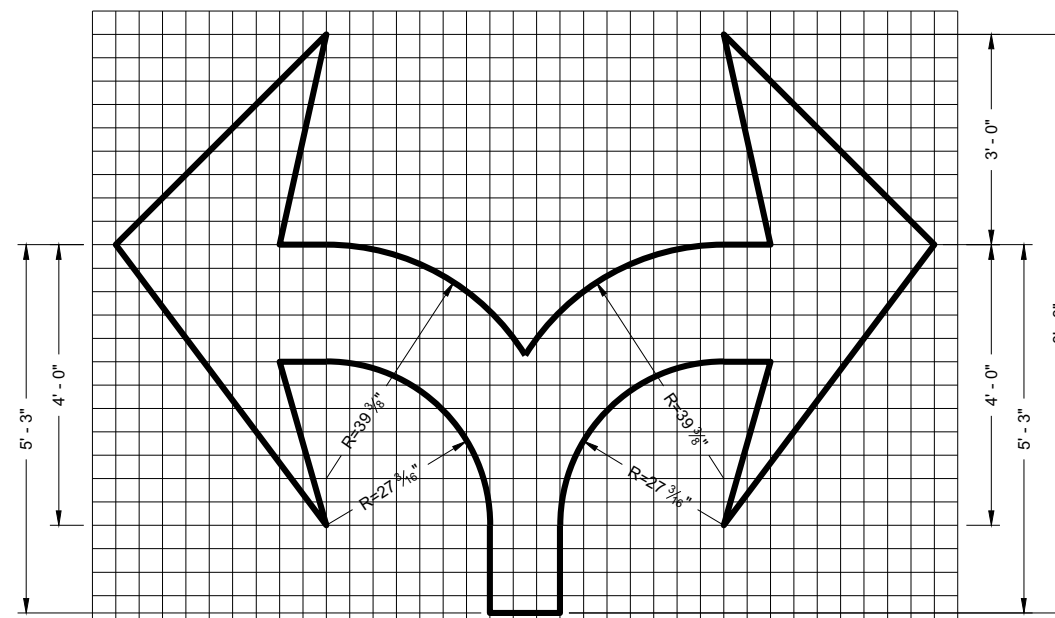
TYPE 4



TYPE 5 LANE DROP ARROW



TYPE 6



TYPE 7

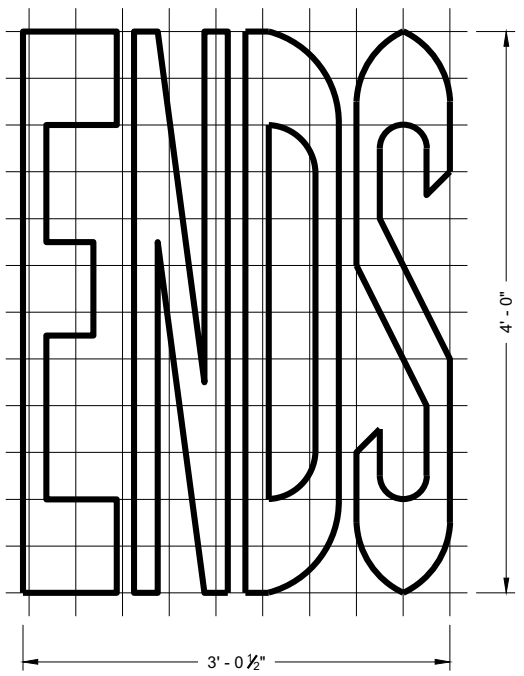
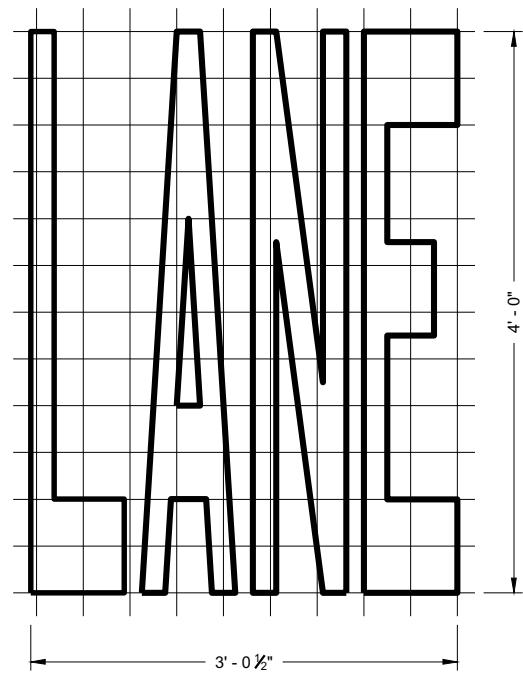
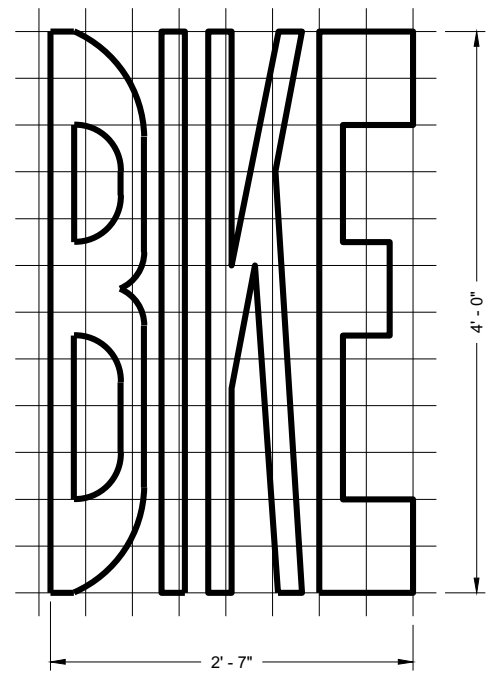
GENERAL NOTES

DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

PAVEMENT MARKING ARROWS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

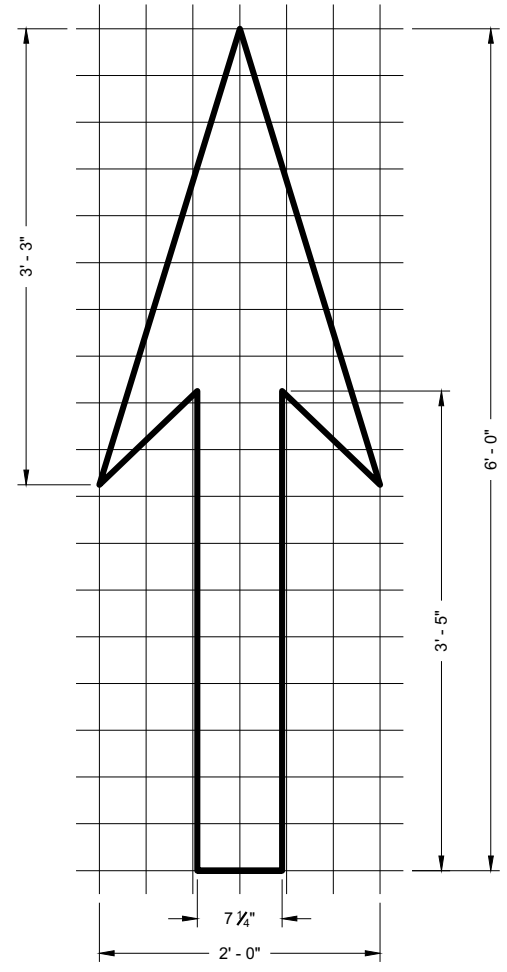
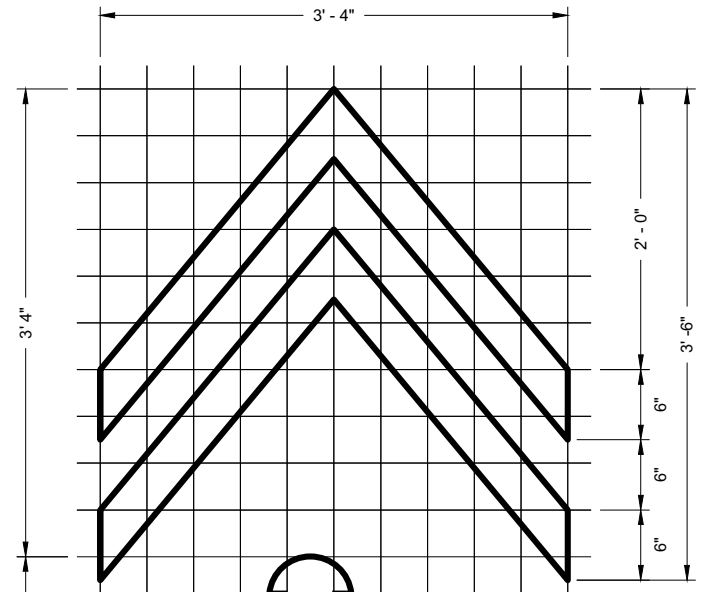
APPROVED
November 2019
DATE /S/ Matthew Rauch
STATE SIGNING AND MARKING
ENGINEER
FHWA



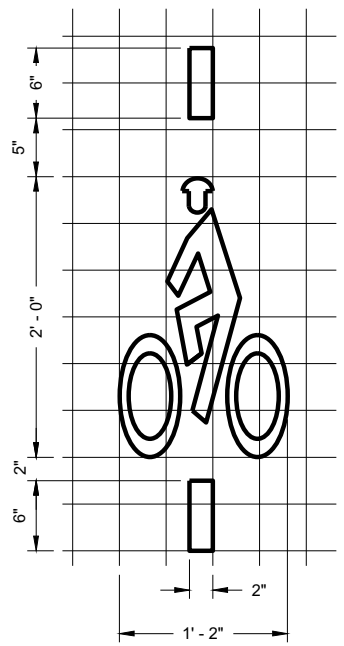
BIKE LANE WORDS

GENERAL NOTES

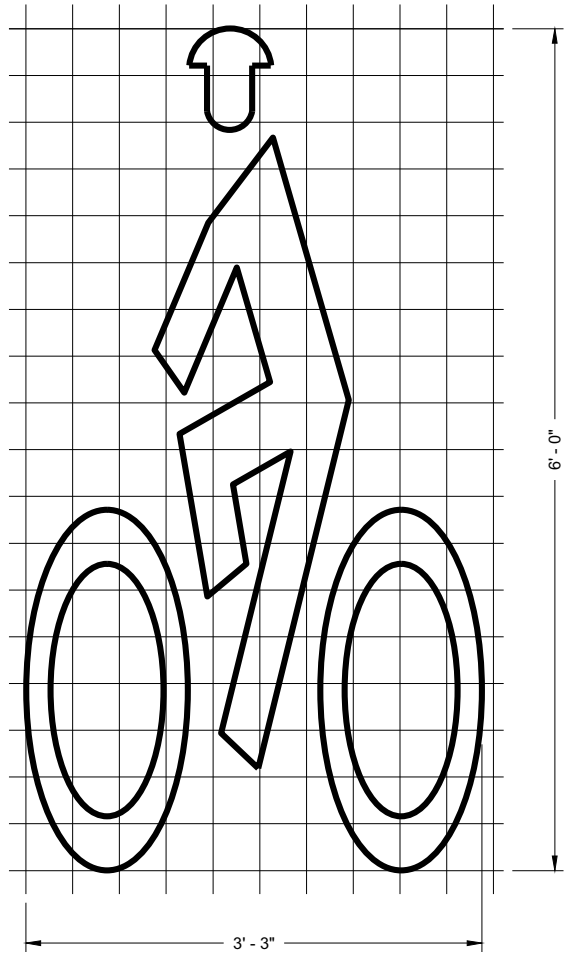
DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.



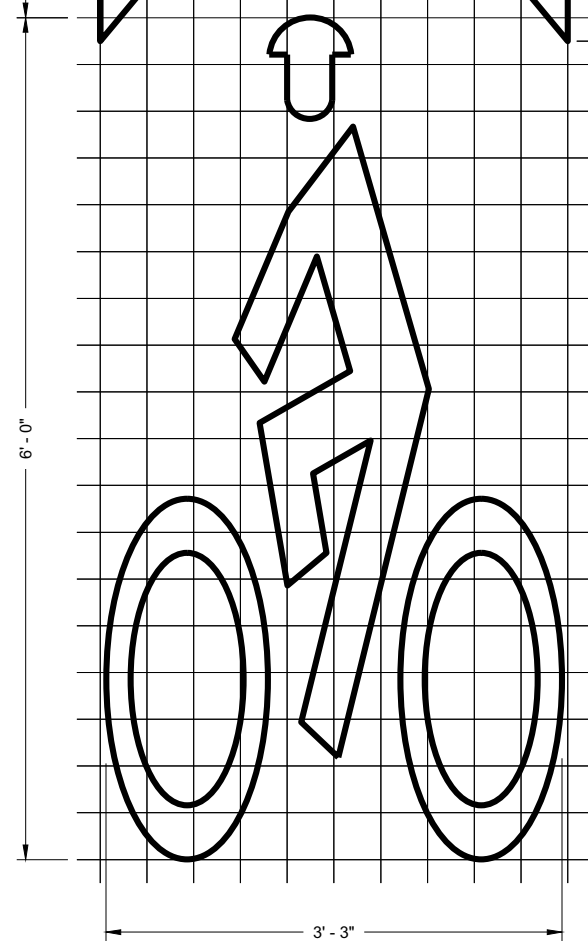
BIKE LANE ARROW



BIKE DETECTOR PAVEMENT MARKING



BIKE LANE SYMBOL



BIKE LANE SYMBOL FOR SHARED LANE

6

6




PAVEMENT MARKING FOR BIKE LANES	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED November 2019 DATE	/s/ Matthew Rauch STATE SIGNING AND MARKING ENGINEER
FHWA	

GENERAL NOTES

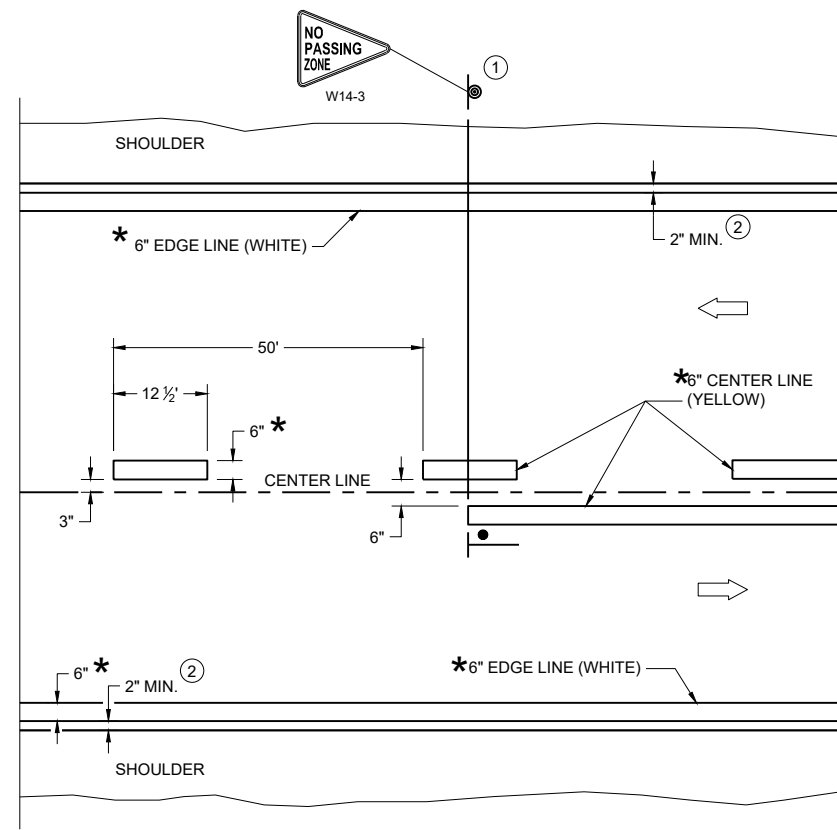
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

- ① LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING
- ② MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

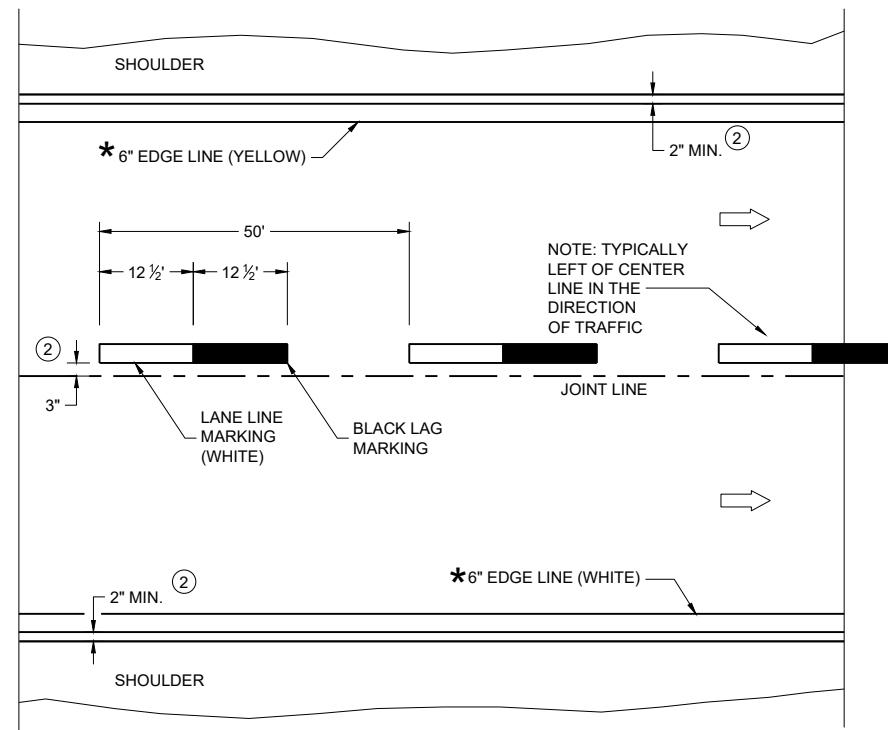
LEGEND

-  "T" MARKING
-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC

*CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES



TWO WAY TRAFFIC



ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING

6

6

SDD 15C08-23a

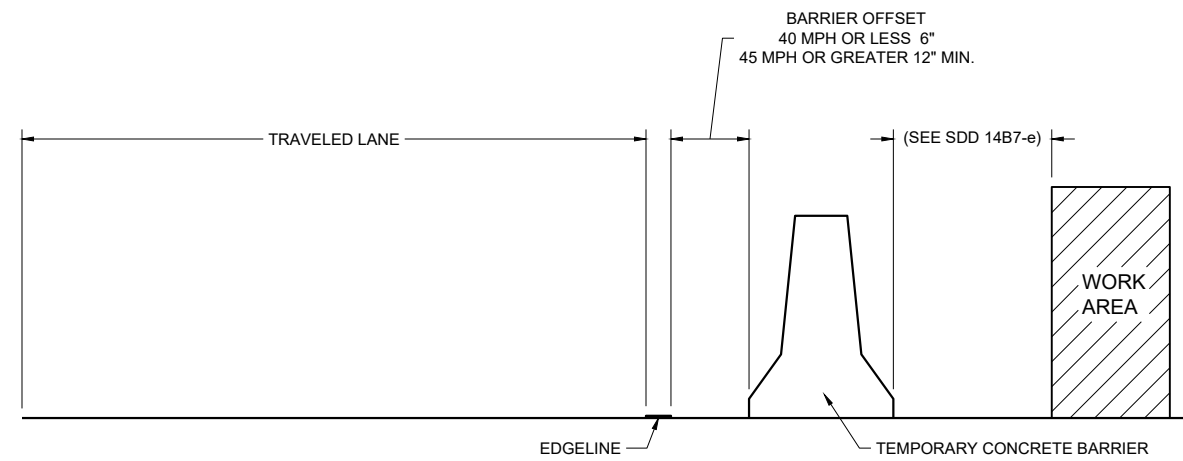
SDD 15C08-23a

PERMANENT LONGITUDINAL PAVEMENT MARKINGS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 /S/ Jeannie Silver
DATE Statewide Pavement Marking Engineer

FHWA



TEMPORARY BARRIER OFFSET FROM EDGELINE

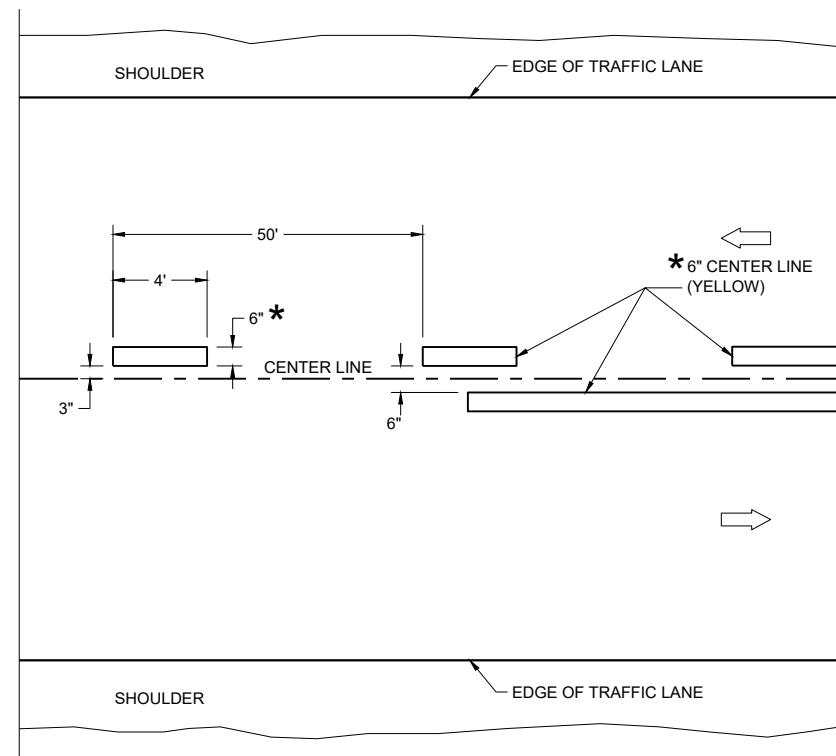
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

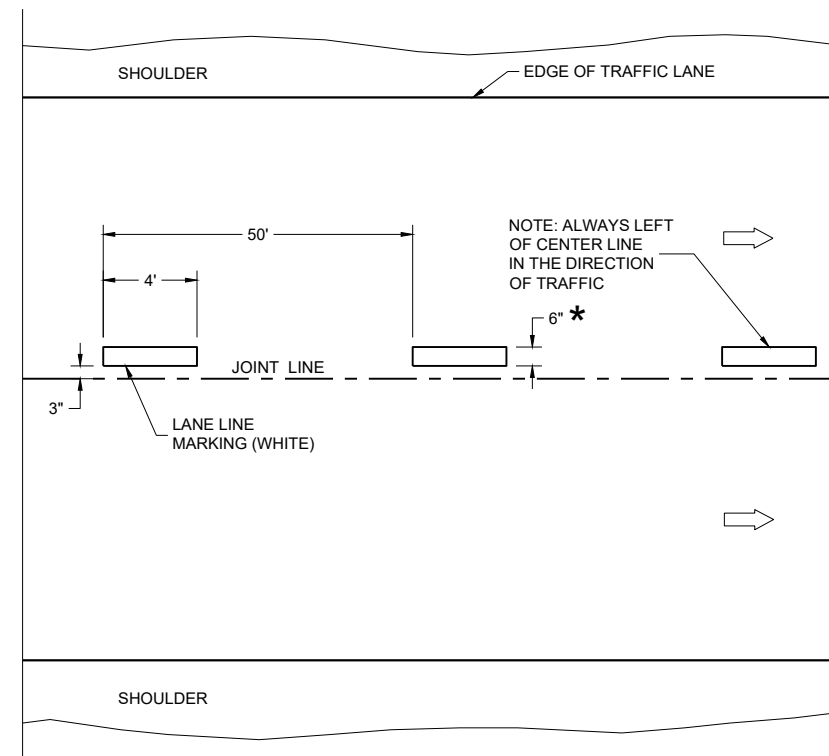
LEGEND

➡ DIRECTION OF TRAFFIC

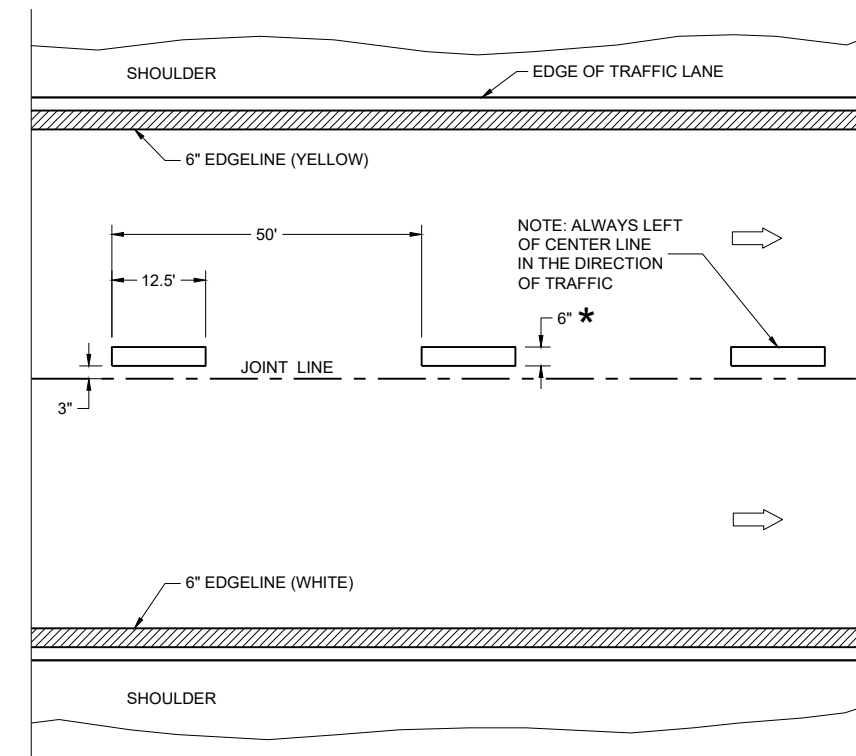
*CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES



TWO WAY TRAFFIC



ONE WAY TRAFFIC



FREEWAYS AND EXPRESSWAYS

TEMPORARY PAVEMENT MARKING

TEMPORARY LONGITUDINAL PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 /S/ Jeannie Silver
DATE Statewide Pavement Marking Engineer

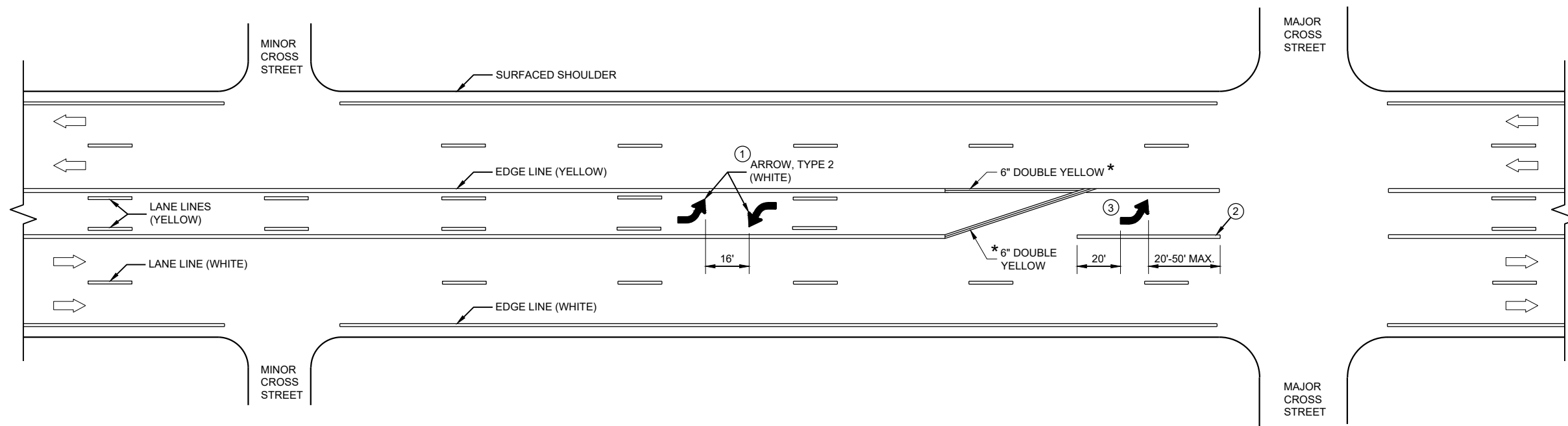
FHWA

GENERAL NOTES

- ① A SET OF ARROWS IS REQUIRED EVERY 400 FEET OR NEAR INTERSECTIONS OR DRIVEWAYS WITH TURNING TRAFFIC.
- ② 10" WHITE
- ③ TURN BAY LENGTH OF LESS THAN 48' DOES NOT REQUIRE PAVEMENT ARROWS OR TEXT.

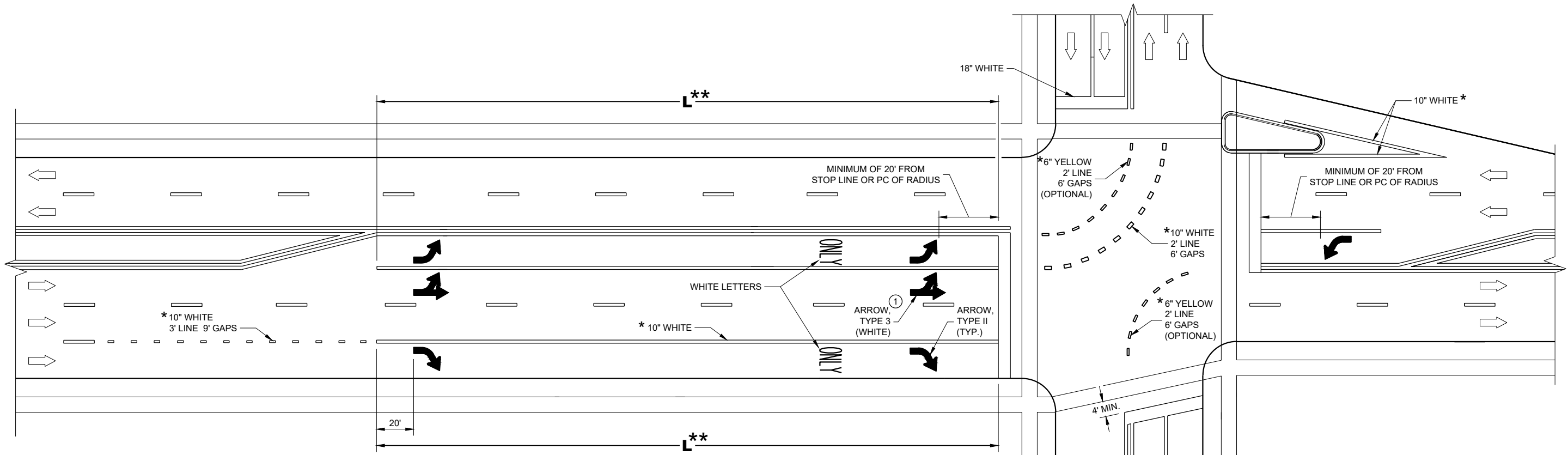
➡ DIRECTION OF TRAFFIC

* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES



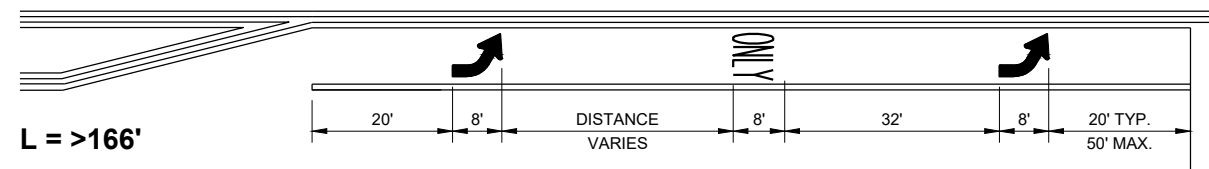
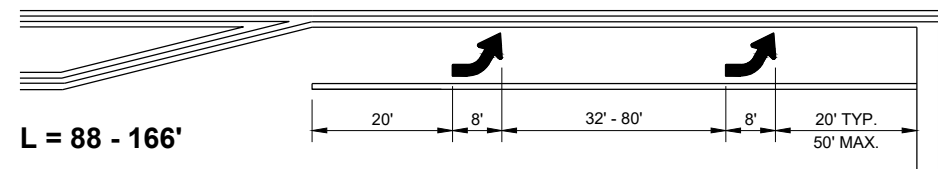
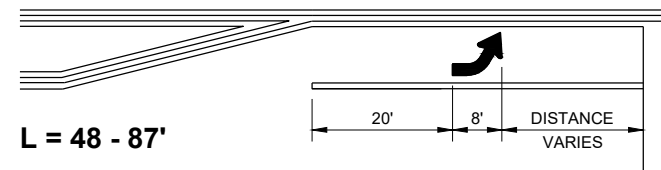
TWO WAY LEFT TURN LANE

PAVEMENT MARKING (TURN LANES)
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION



TURN LANE OPTIONS

LENGTH OF TURN BAY (**L**) OF 0 - 47' DOES NOT REQUIRE PAVEMENT MARKING ARROWS OR WORDS



** (SEE TURN LANE OPTIONS FOR PLACEMENT OF PAVEMENT MARKING ARROWS AND WORDS)

GENERAL NOTES

① QUANTITY AND LOCATION OF TYPE 3 ARROWS ARE THE SAME AS THE TYPE II ARROWS IN THE ADJACENT TURN LANE. FOR TURN LANES WITH A PHYSICAL SEPARATION IN THE SAME DIRECTION OF TRAVEL, THE ARROWS AND "ONLY" MARKING MAY BE ELIMINATED.

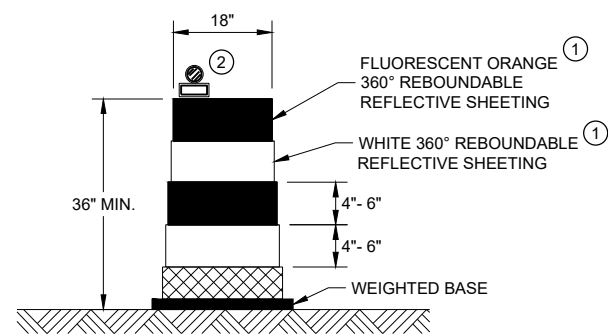
➡ DIRECTION OF TRAFFIC

L = LENGTH OF TURN BAY

* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES

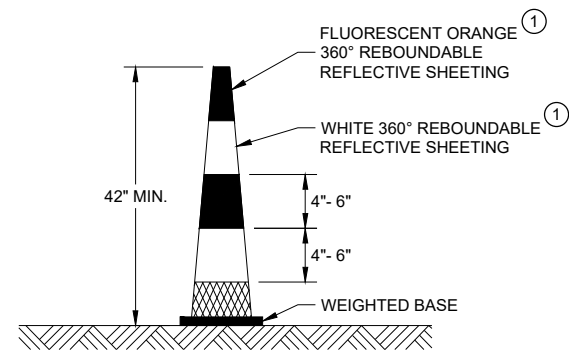
PAVEMENT MARKING (TURN LANES)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



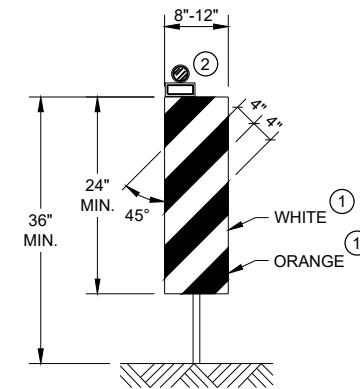
DRUM

BALLAST WIDTHS
RANGE FROM 24"-36"



42" CONE

DO NOT USE IN TAPERS
½ SPACING OF DRUMS
BALLAST WIDTHS
RANGE FROM 14"-20"

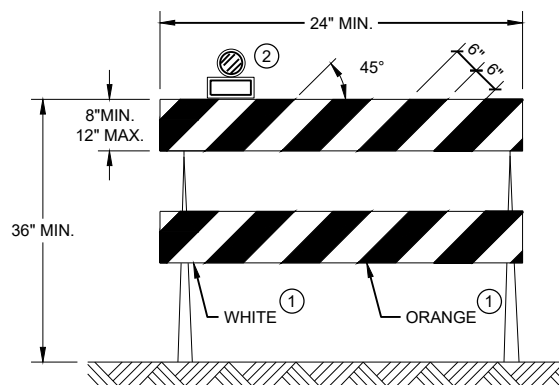


VERTICAL PANEL

THE STRIPES SHALL SLOPE DOWNWARD TO
THE TRAFFIC SIDE FOR CHANNELIZATION.

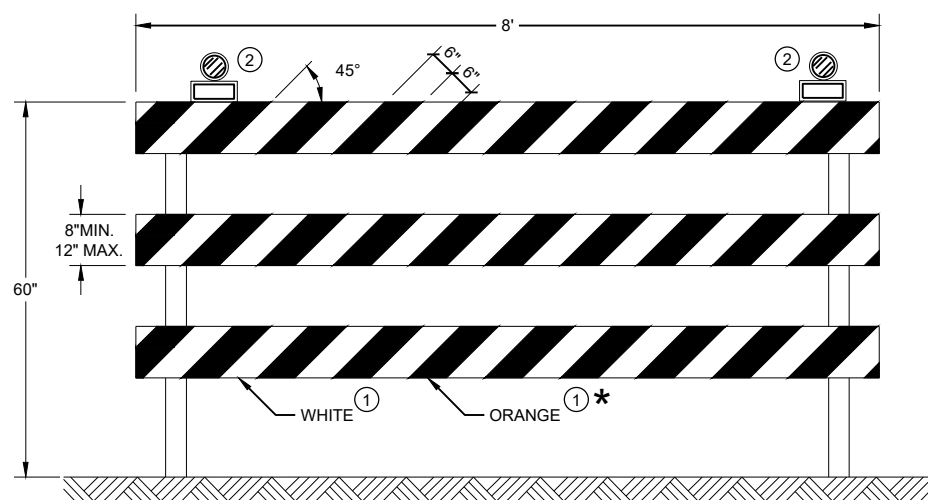
GENERAL NOTES

- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.



TYPE II BARRICADE

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES
MAY BE USED. ALL STRIPES SHALL SLOPE DOWNWARD
TO THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE III BARRICADE

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP
TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.

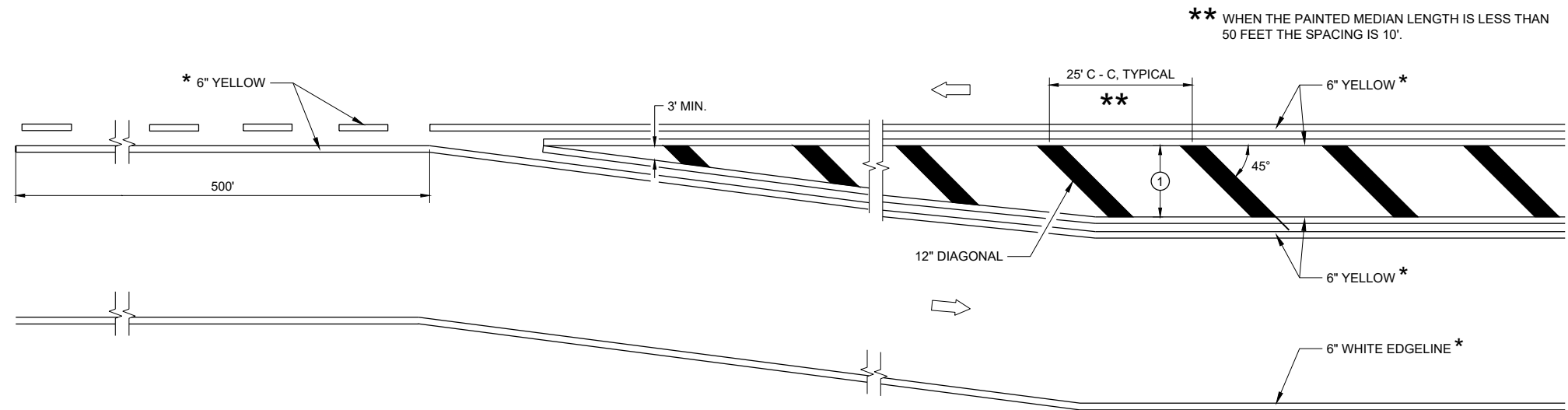
* IF USED FOR A PERMANENT APPLICATION USE RED SHEETING.

**CHANNELIZING DEVICES
DRUMS, CONES, BARRICADES
AND VERTICAL PANELS**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2022 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA



MEDIAN ISLAND DETAIL

****** WHEN THE PAINTED MEDIAN LENGTH IS LESS THAN 50 FEET THE SPACING IS 10'.

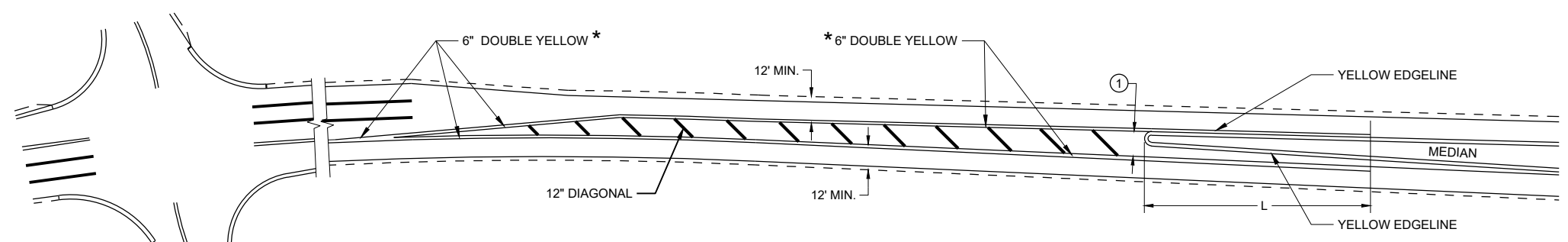
GENERAL NOTES

① DIAGONALS ARE OPTIONAL WHEN PAINTED ISLAND IS LESS THAN 6 FEET AT THE WIDEST POINT. OMIT DIAGONALS IF WIDTH IS LESS THAN 4 FEET.

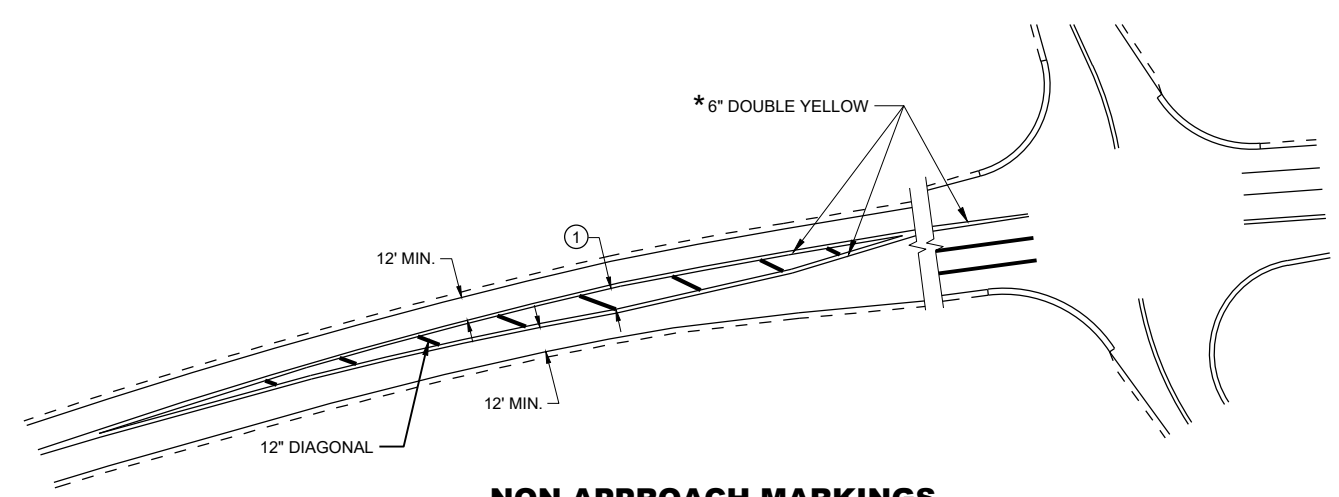
➔ DIRECTION OF TRAVEL

* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES

SPEED LIMIT	L
<35 MPH	5'
35> MPH	50'



APPROACH MARKINGS FOR OTHER MEDIAN TYPES



NON-APPROACH MARKINGS

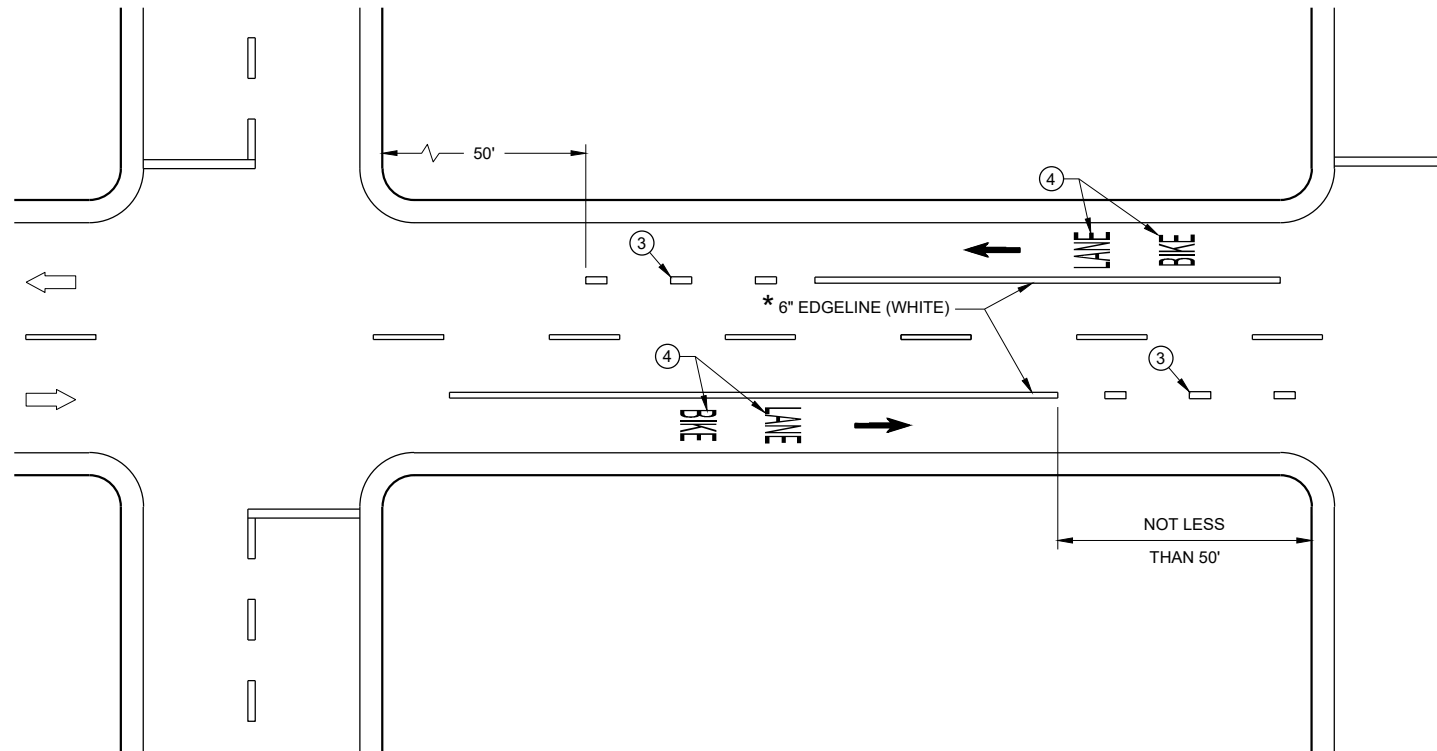
6

6

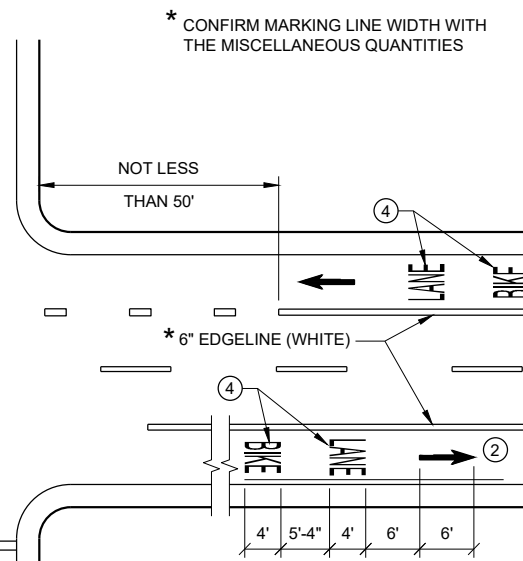
SDD 15C18-08a

SDD 15C18-08a

MEDIAN ISLAND PAVEMENT MARKINGS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2023 DATE	/s/ Jeannie Silver ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR
FHWA	

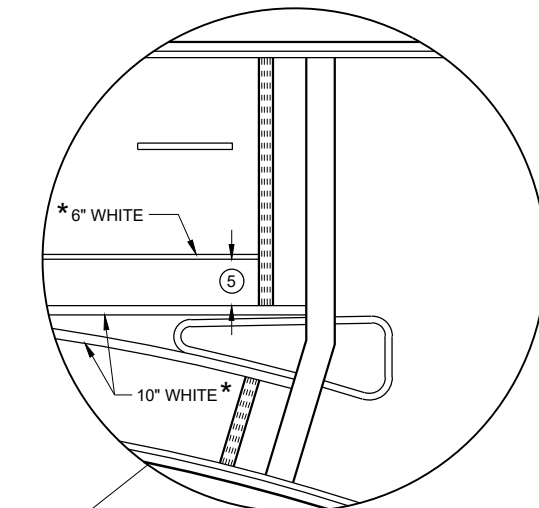
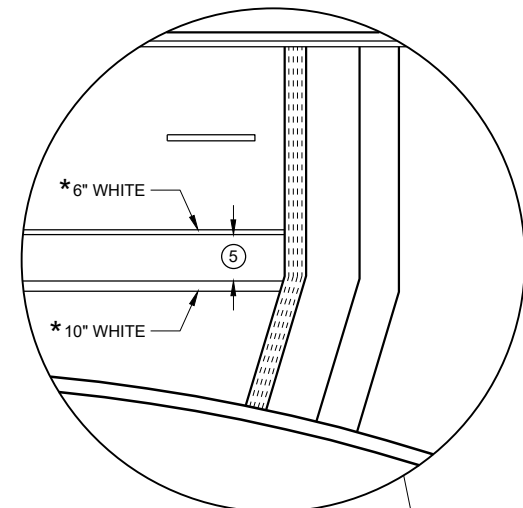


DESIGNATED BIKE LANE - NO PARKING

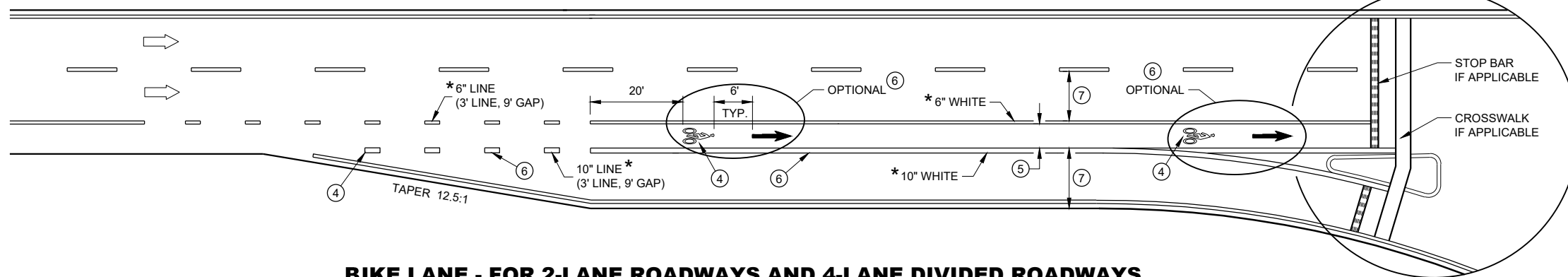


4 LANE DIVIDED WITHOUT ISLAND

- GENERAL NOTES**
- 1 DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.
 - 2 MINIMUM OF ONE PER BLOCK. MAXIMUM OF 250 FEET.
 - 3 DOTTED LINES (3' LINE, 9' GAP) SHOULD BE USED 50 FEET TO 200 FEET IN ADVANCE OF AN INTERSECTION WHERE THERE IS NO RIGHT TURN ONLY LANE AND THERE IS HEAVY RIGHT TURN TRAFFIC OR THERE IS A NEAR-SIDE BUS STOP. AT OTHER INTERSECTIONS WHERE RIGHT TURN TRAFFIC IS LIGHT TO MODERATE, A SOLID LINE CAN BE USED UP TO THE INTERSECTION.
 - 4 BIKE SYMBOLS OR WORDS MAY BE USED.
 - 5 BIKE ACCOMMODATION IS TYPICAL 5 FEET WIDE AND MINIMUM OF 4 FEET FROM A LONGITUDINAL JOINT. USE 5 FEET AT ≥ 45 MPH.
 - 6 OMIT THESE MARKINGS FOR WIDER TURN LANE APPLICATIONS (MINIMUM OF 15 FOOT WIDE TURN LANE).
 - 7 REFER TO CONTRACT PLANS FOR LANE WIDTH.
- DIRECTION OF TRAVEL →



4 LANE DIVIDED WITH ISLAND



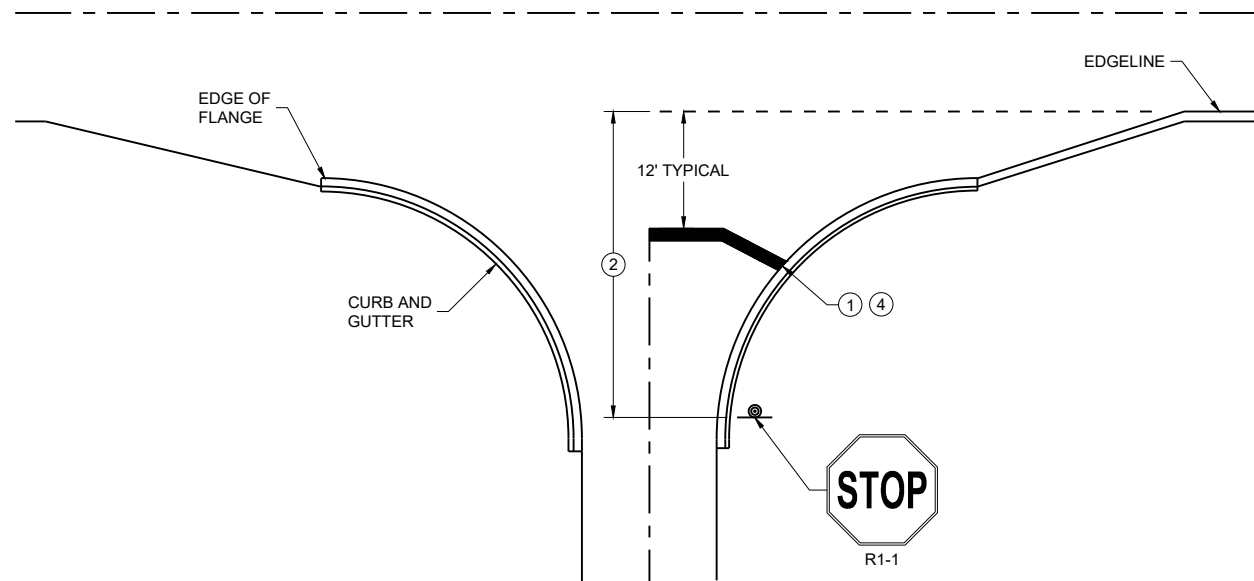
BIKE LANE - FOR 2-LANE ROADWAYS AND 4-LANE DIVIDED ROADWAYS (4-LANE DIVIDED WITH RIGHT TURN LANE SHOWN)

BIKE LANE MARKING	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2023 DATE	/S/ Matthew Rauch STATE SIGNING AND MARKING ENGINEER
FHWA	

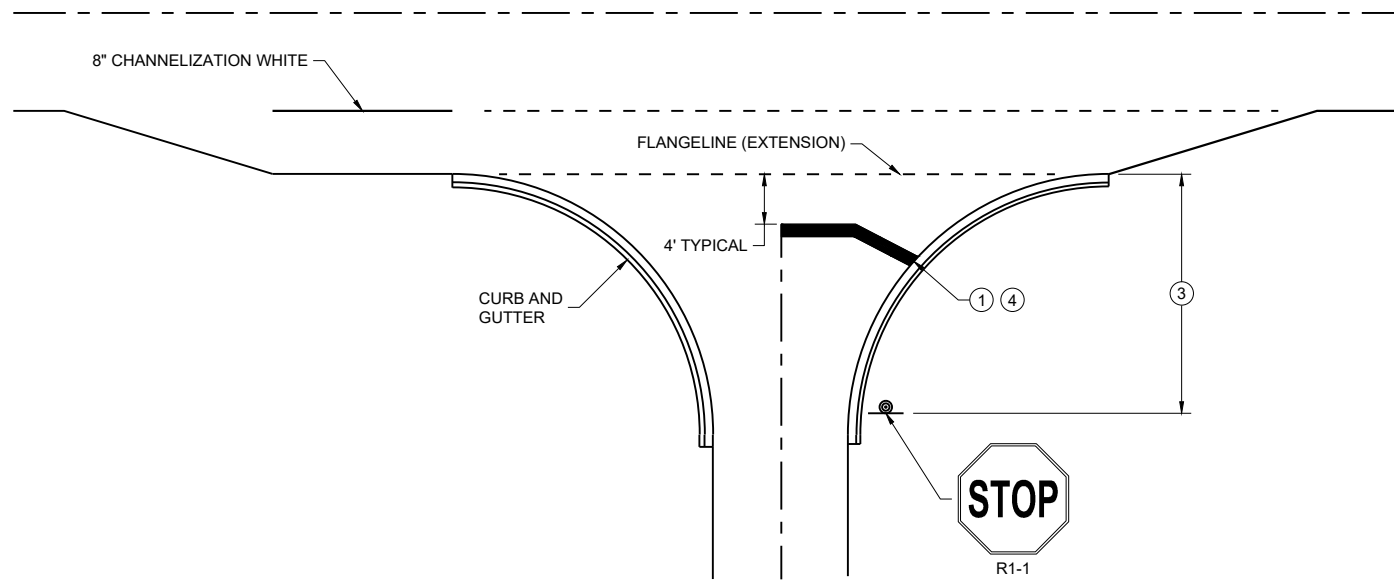
GENERAL NOTES

STOP SIGN SHALL BE PLACED A MINIMUM OF 6 FEET TO A MAXIMUM OF 50 FEET FROM THE EDGELINE LOCATION.

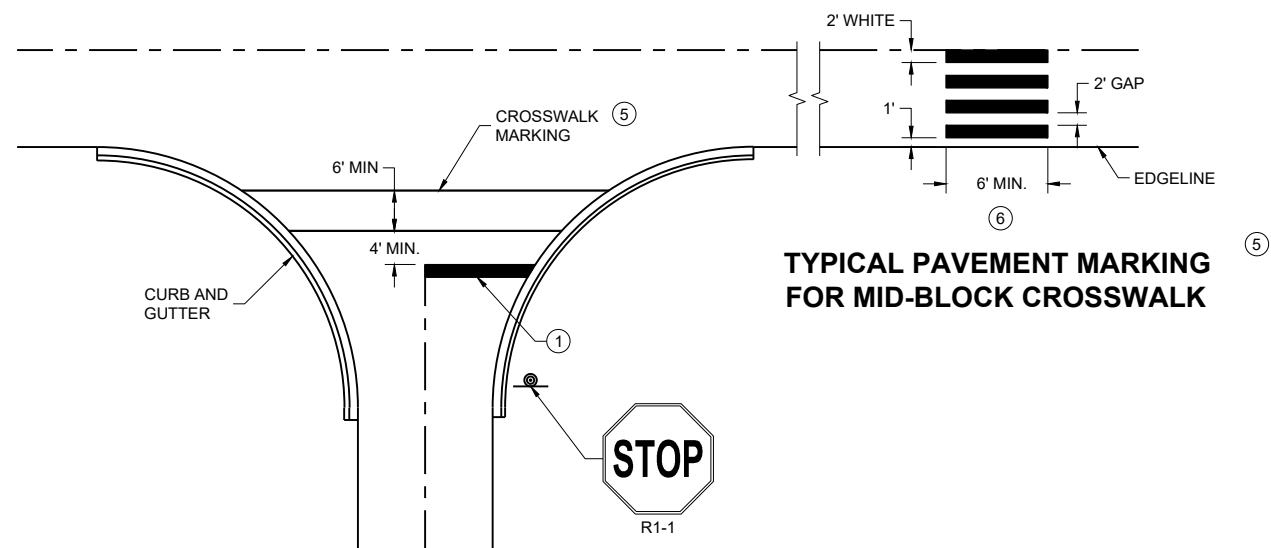
- ① 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE REGION MARKING ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
- ② NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGELINE.
- ③ NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGE LINE EXTENSION.
- ④ MOVE CLOSER TO THE EDGE OF TRAVEL LINE AS NEEDED FOR VISIBILITY AND SIGHT LINES (NO CLOSER THAN 4 FEET).
- ⑤ LADDER BAR CROSSWALKS SHOULD ONLY BE USED FOR MID BLOCK CROSSINGS. USE 2 - 6" TRANSVERSE LINES.
- ⑥ POSTED SPEED LIMITS OF 40 MPH OR GREATER USE A MINIMUM WIDTH OF 8' FOR MIDBLOCK CROSSWALKS



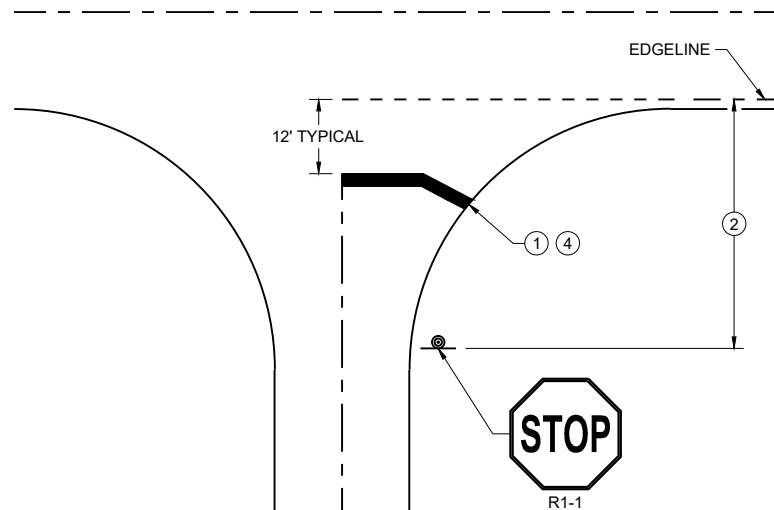
TYPICAL STOP LINE PAVEMENT MARKING WITH CURB AND GUTTER



TYPICAL STOP LINE PAVEMENT MARKING FOR SIDE ROADS WITH RIGHT TURN LANE



TYPICAL STOP LINE PAVEMENT MARKING FOR SIDE ROADS WITH CROSSWALK MARKING



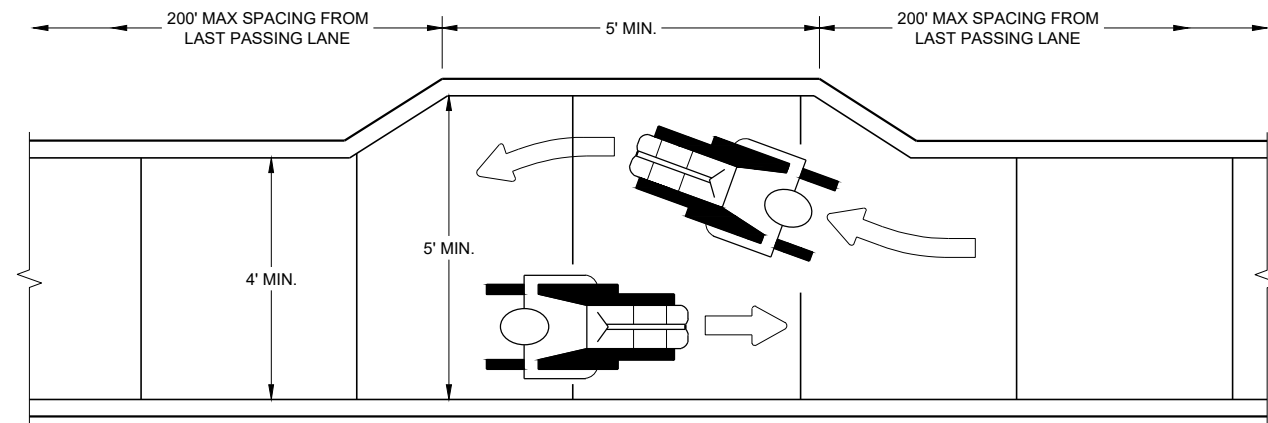
TYPICAL STOP LINE PAVEMENT MARKING WITHOUT CURB AND GUTTER

STOP LINE AND CROSSWALK PAVEMENT MARKING

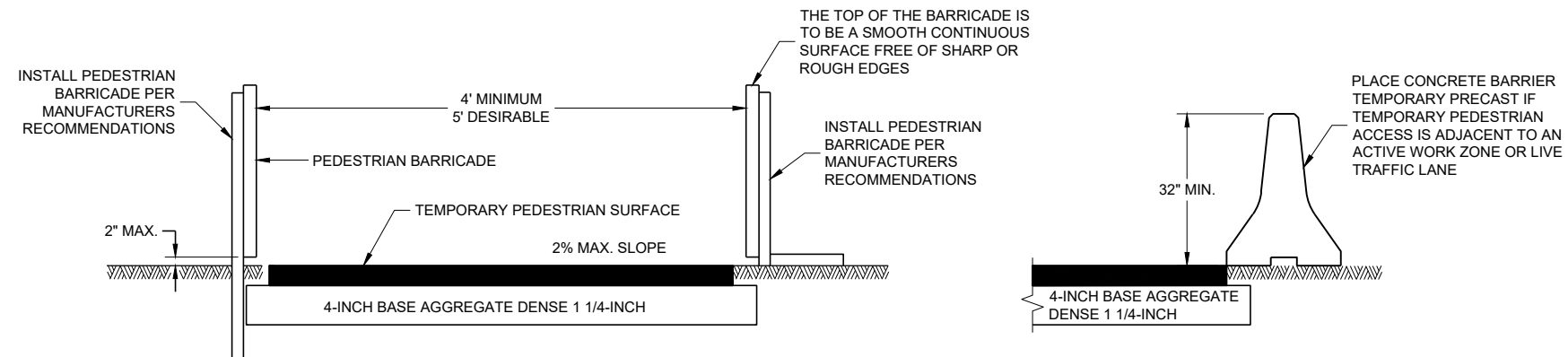
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
March 2024 /s/ Matthew Rauch
DATE STATE SIGNING AND MARKING
ENGINEER

FHWA



NARROW SIDEWALK PASSING DETAIL



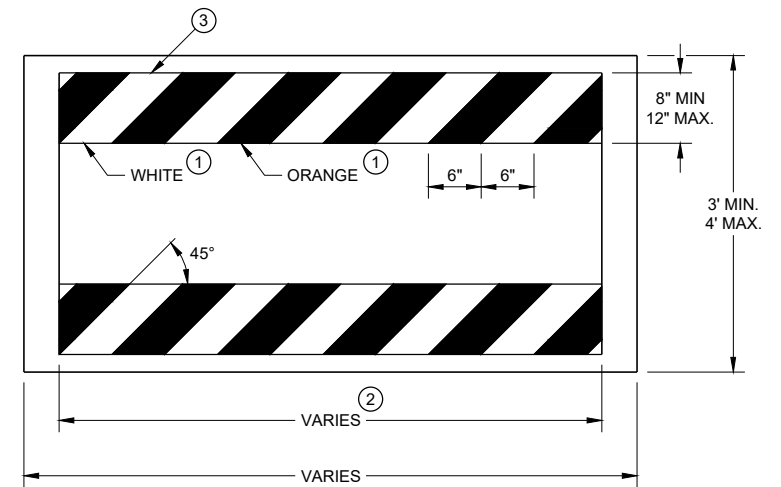
TEMPORARY PEDESTRIAN ACCESS

GENERAL NOTES

BARRICADE DEVICE SELECTED FROM APPROVED PRODUCT LIST

- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② SHEETING REQUIRED ON MORE THAN 50% OF BARRICADE WIDTH.
- ③ PLACE SHEETING ON BOTH SIDES OF THE BARRICADE.

* USE THIS DETAIL FOR SHEETING PLACEMENT REFERENCE.



TEMPORARY PEDESTRIAN BARRICADE*

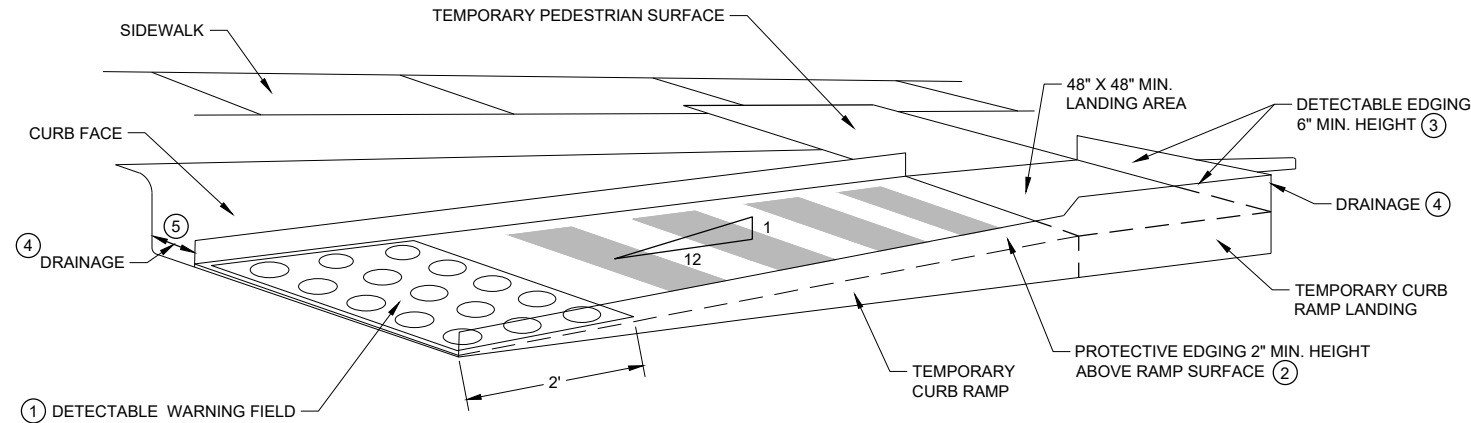
**TRAFFIC CONTROL,
PEDESTRIAN
ACCOMMODATION**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

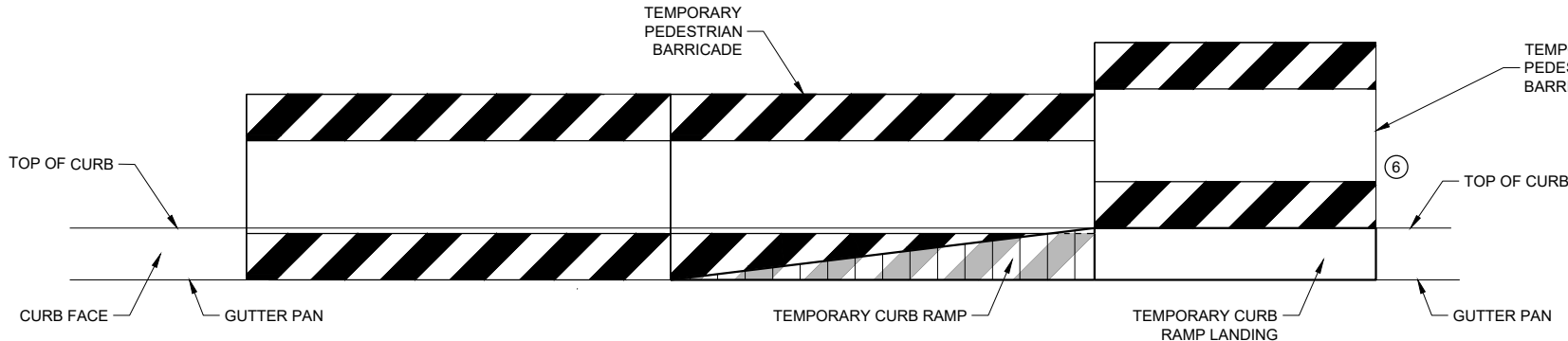
GENERAL NOTES

CURB RAMPS SHALL BE 48" MIN. WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE.
 CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.
 CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.
 LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.
 CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES MAY BE VERTICAL UP TO 1/4" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".

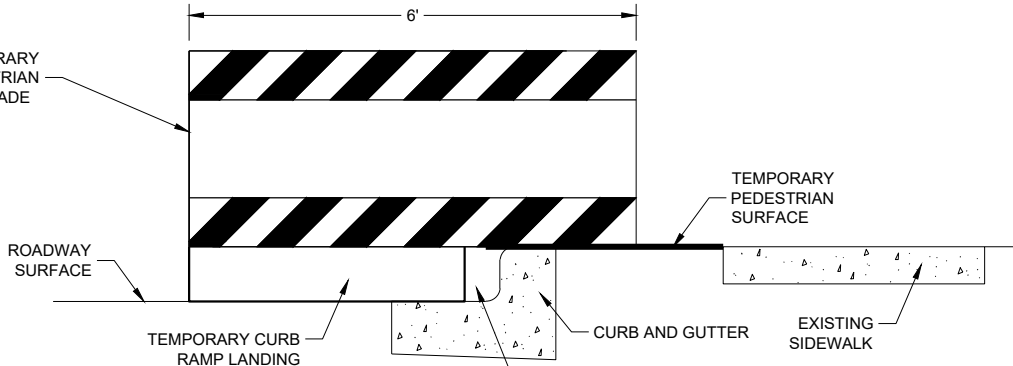
- ① INSTALL CONTRASTING TEMPORARY DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS, AS SHOWN IN THE PLANS.
- ② PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- ③ DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- ④ DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ⑤ ENSURE CURB RAMP IS OUT OF THE GUTTER PAN.
- ⑥ IF ONLY PART OF THE END PANEL OF TEMPORARY PEDESTRIAN BARRICADE PANEL IS NEEDED, EXTEND EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL HERE.



PERSPECTIVE VIEW



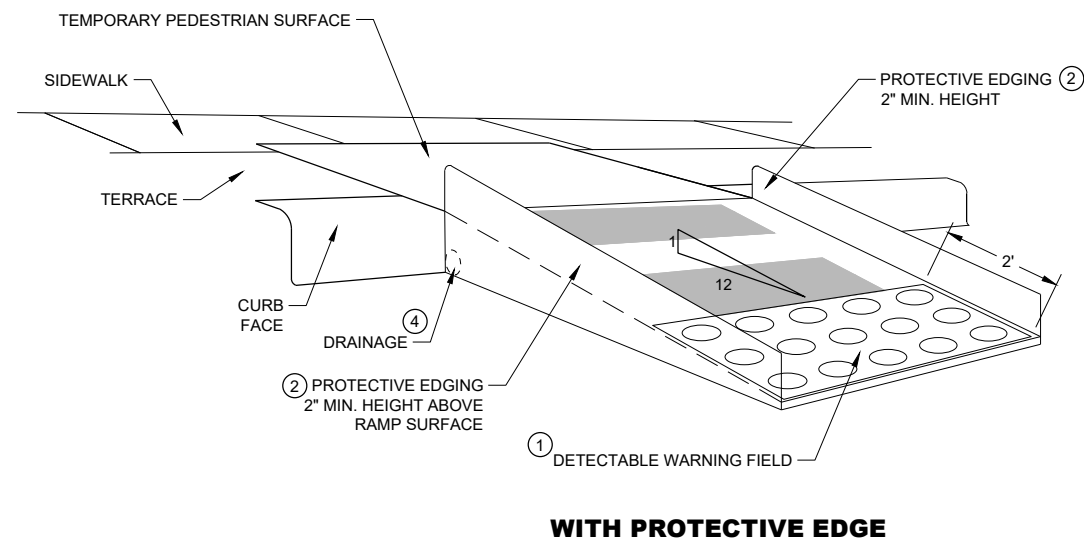
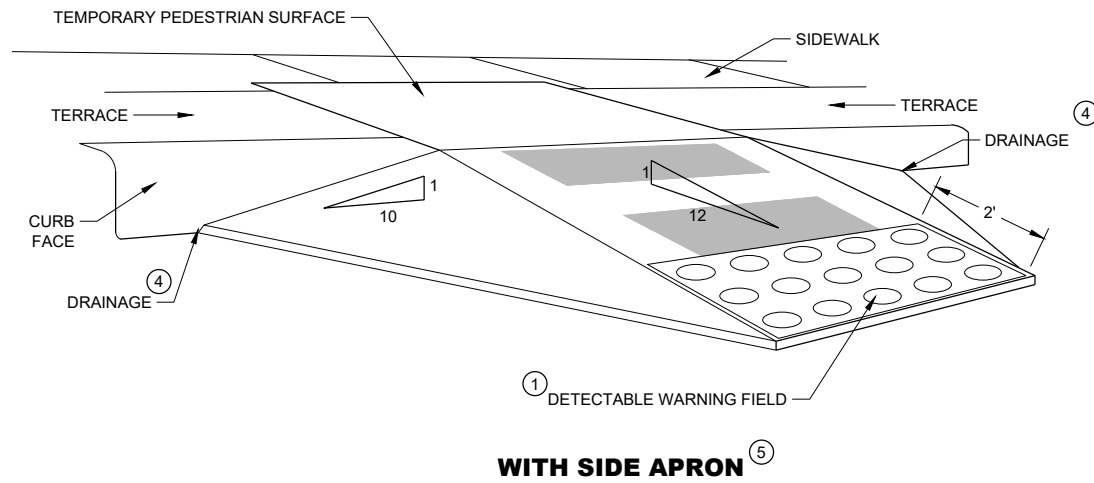
FRONT VIEW



SIDE VIEW

TEMPORARY CURB RAMP PARALLEL TO CURB

<p>TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION</p>
<p>STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION</p>



TEMPORARY CURB RAMP PERPENDICULAR TO CURB

GENERAL NOTES

CURB RAMPS SHALL BE 48" MINIMUM WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE.

ALTERNATE SIDEWALK WORK BETWEEN LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.

CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.

CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.

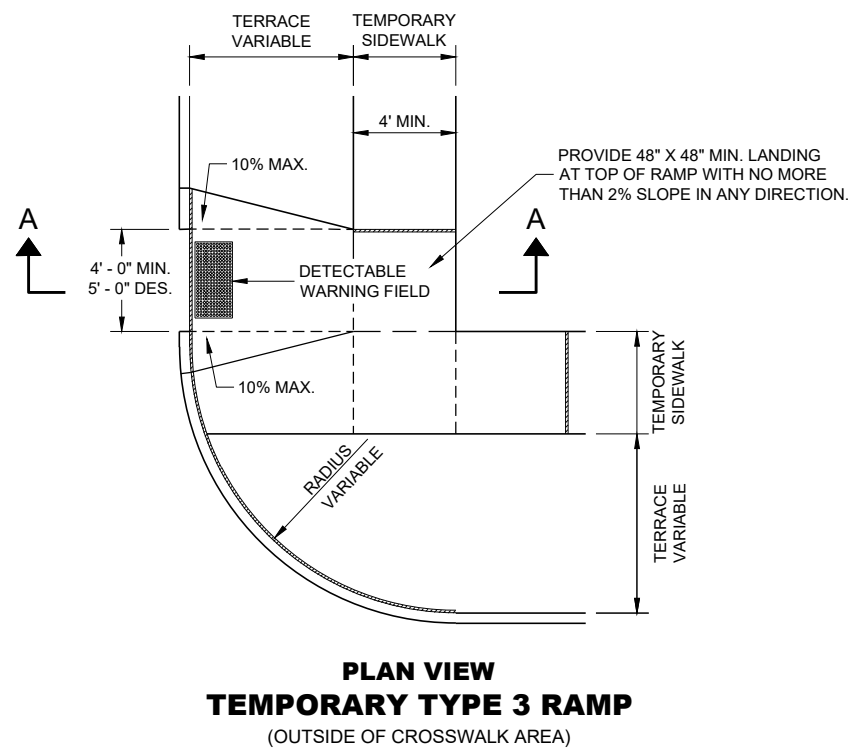
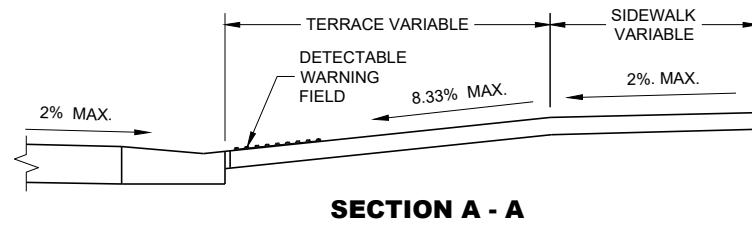
LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.

CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES MAY BE VERTICAL UP TO 1/4" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".

- ① INSTALL CONTRASTING TEMPORARY DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS, AS SHOWN IN THE PLANS
- ② PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- ③ DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- ④ DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ⑤ CAN ONLY BE USED FOR RAMPS WITH 6" OR LESS OF VERTICAL CHANGE.

GENERAL NOTES

- BARRICADE DEVICE SELECTED FROM APPROVED PRODUCT LIST
- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② SHEETING REQUIRED ON MORE THAN 50% OF BARRICADE WIDTH.
- ③ PLACE SHEETING ON BOTH SIDES OF THE BARRICADE.
- ★ USE THIS DETAIL FOR SHEETING PLACEMENT REFERENCE.



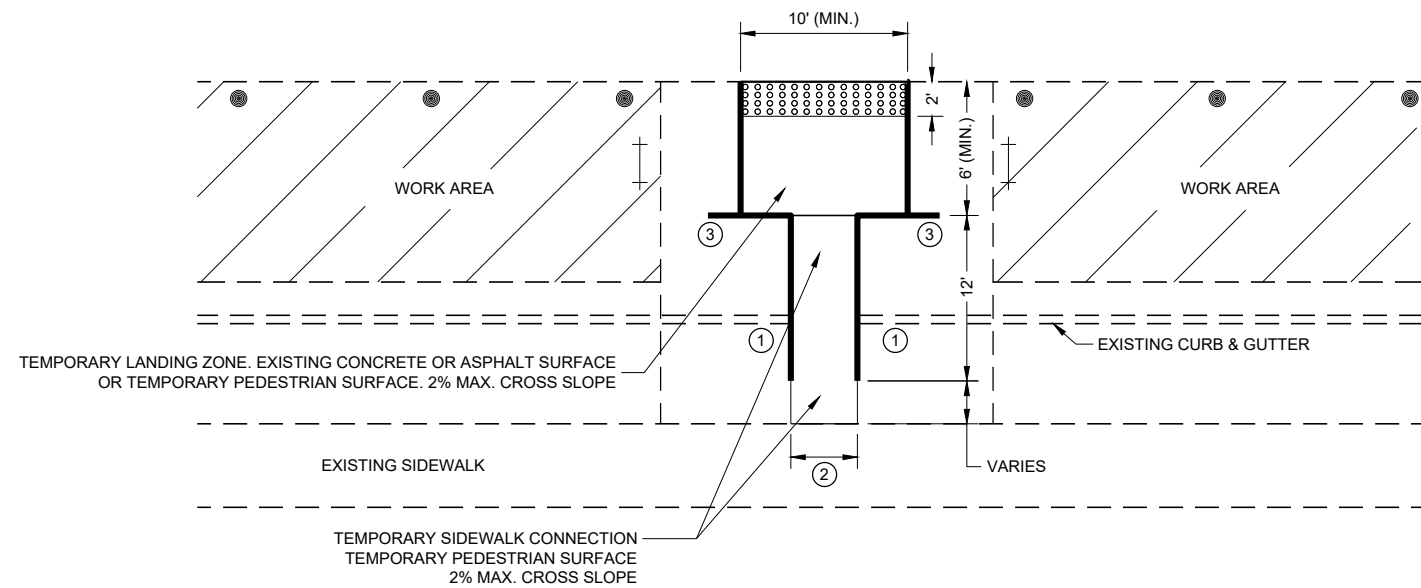
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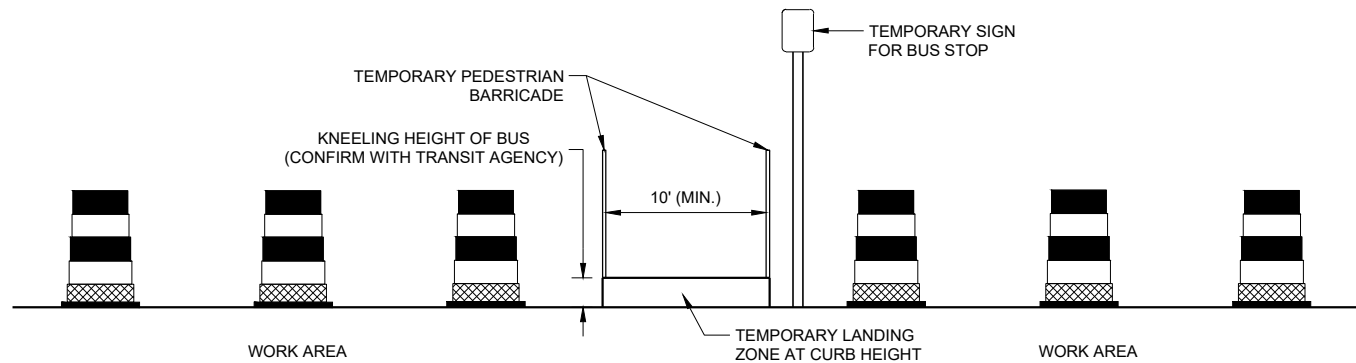
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SDD 15D30-09d

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2023 DATE	/s/ Andrew Heidtke WORK ZONE ENGINEER
<small>FHWA</small>	



PLAN VIEW



PROFILE VIEW
TEMPORARY BUS STOP PAD

GENERAL NOTES

- TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.
- NOTIFY THE BUS COMPANY 7 DAYS IN ADVANCE OF THE BUS STOP RELOCATION.
- PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMP OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.
- CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES MAY BE VERTICAL UP TO 1/4" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".
- CURB RAMP AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.

- ① DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ② 5' WIDE MIN. WITH TEMPORARY PEDESTRIAN BARRICADE, 10' WIDE MIN. WITHOUT TEMPORARY PEDESTRIAN BARRICADE.
- ③ PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE INTO THIS SPACE.







LEGEND

- TRAFFIC CONTROL DRUM
- ⊥ TYPE III BARRICADE
- TEMPORARY PEDESTRIAN BARRICADE
- ◻ TEMPORARY DETECTABLE WARNING FIELD
- ▨ WORK AREA

**TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION**

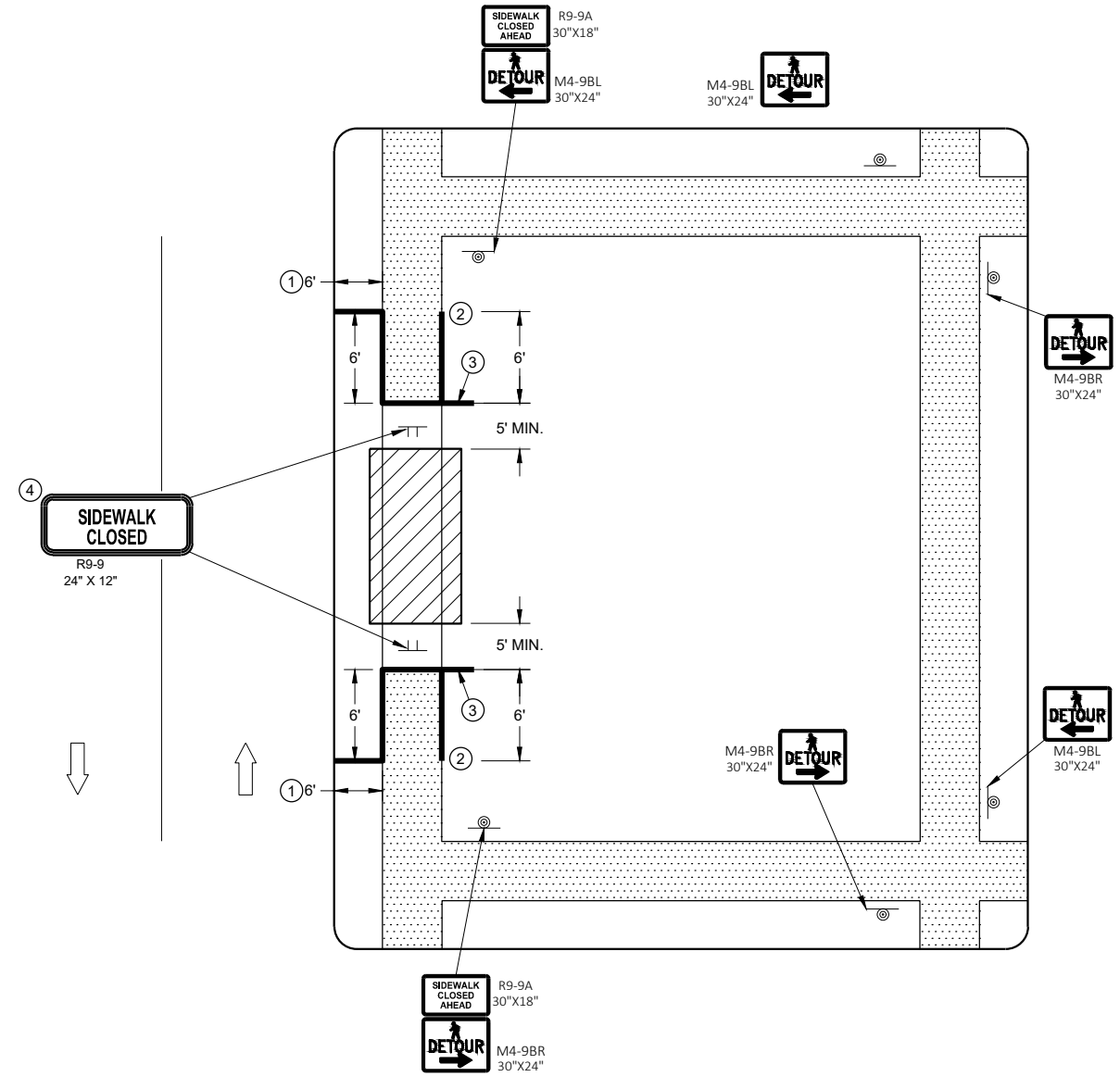
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  SIGN ON TEMPORARY SUPPORT
-  UNDER PEDESTRIAN TRAFFIC
-  WORK AREA
-  TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC

GENERAL NOTES

- SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.
- WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.
- SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.
- PLACE TEMPORARY PEDESTRIAN BARRICADE TO FIT FIELD CONDITIONS, AVOIDING CONFLICTS WITH DRIVEWAYS AND OTHER EXISTING FEATURES.
- ① IF TERRACE IS LESS THAN 6 FEET WIDE, OMIT TEMPORARY PEDESTRIAN BARRICADE FROM THE SIDEWALK TO THE CURB.
 - ② PLACE BARRICADE CLOSURE SO THAT THE TEMPORARY PEDESTRIAN BARRICADE END IS AT THE LAST OPEN SIDEWALK ACCESS TO RESIDENCES OR BUSINESSES BEFORE THE SIDEWALK CLOSURE.
 - ③ IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
 - ④ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.



SIDEWALK DETOUR, SIDEWALK ONLY ON ONE SIDE

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION










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SDD 15D30 - 09f

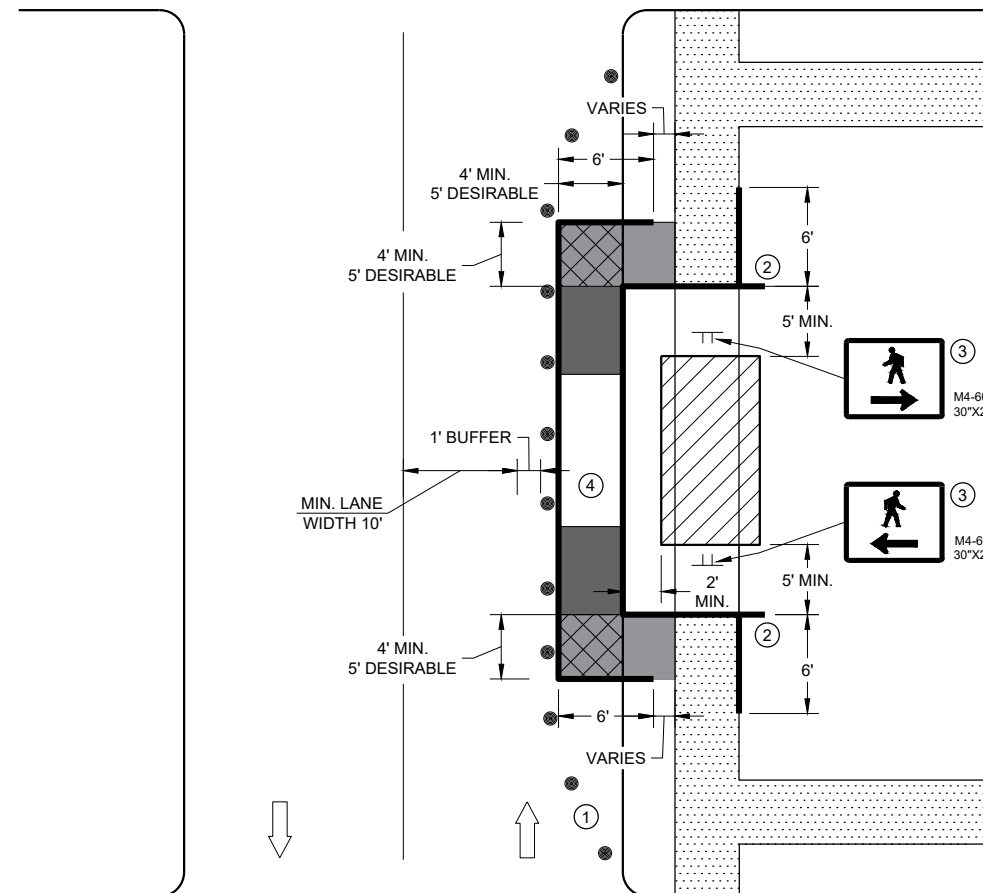
SDD 15D30 - 09f

LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL DRUM
-  WORK AREA
-  UNDER PEDESTRIAN TRAFFIC
-  TEMPORARY CURB RAMP
-  TEMPORARY PEDESTRIAN SURFACE "A"
-  TEMPORARY PEDESTRIAN SURFACE "B"
-  TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC

GENERAL NOTES

- TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.
- SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.
- WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.
- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND BUFFER SPACE REQUIRED.
 - ② PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL PAST THE SIDEWALK ON THE SIDE AWAY FROM THE ROAD.
 - ③ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.
 - ④ USE EXISTING PAVEMENT SURFACE. IF EXISTING PAVEMENT SURFACE HAS BEEN REMOVED, USE A TEMPORARY PEDESTRIAN SURFACE.



SIDEWALK DIVERSION, SINGLE SIDE

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

IF PEDESTRIAN PUSH BUTTONS ARE PRESENT ON THE EXISTING FACILITY, ENSURE THEY ARE MAINTAINED/ACCESSIBLE FOR PEDESTRIAN USE THROUGHOUT THE TEMPORARY PEDESTRIAN ACCOMMODATIONS.











SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

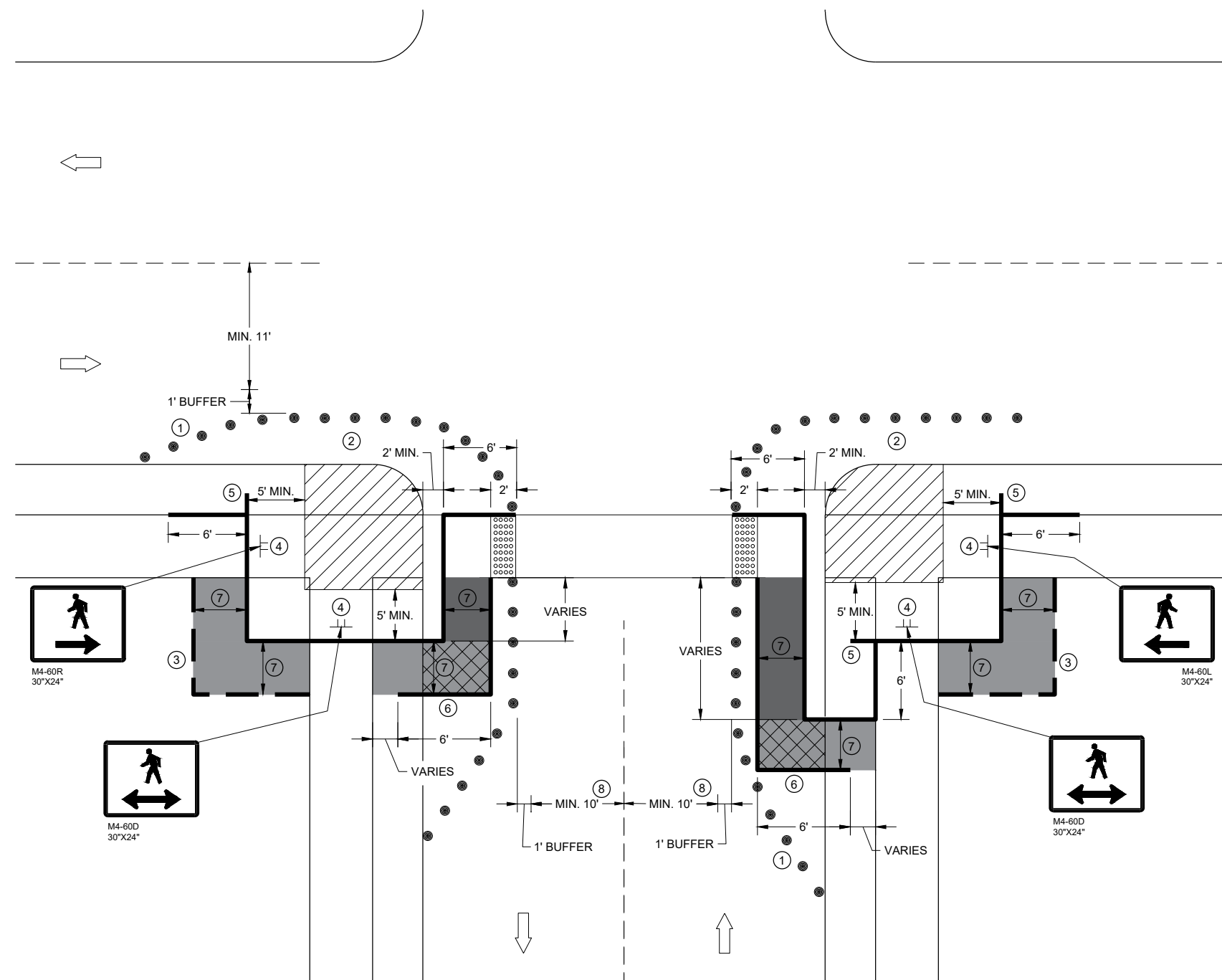
TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG

WHEN TEMPORARY PEDESTRIAN BARRICADE RUNS PARALLEL ALONG THE SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND PROPER BUFFER SPACE REQUIRED.
- ② PROVIDE ADEQUATE SPACE FOR CONTRACTOR OPERATIONS
- ③ USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
- ④ MOUNTING HEIGHT OF 5 FEET FROM SIDEWALK SURFACE TO BOTTOM OF SIGN.
- ⑤ PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL IN THE SIDEWALK TERRACE.
- ⑥ IF TEMPORARY PEDESTRIAN BARRICADE DOES NOT REACH THE FACE OF THE CURB, USE AN ADDITIONAL PANEL AND EXTEND INTO THE TERRACE.
- ⑦ 4 FEET MINIMUM, 5 FEET DESIRABLE
- ⑧ IF MINIMUM LANE WIDTHS CAN'T BE ATTAINED, CURB RAMPS MAY NEED TO BE CONSTRUCTED AT SEPARATE TIMES.

LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL DRUM
-  WORK AREA
-  TEMPORARY CURB RAMP
-  TEMPORARY PEDESTRIAN SURFACE "A"
-  TEMPORARY PEDESTRIAN SURFACE "B"
-  TEMPORARY DETECTABLE WARNING FIELD
-  TEMPORARY PEDESTRIAN BARRICADE
-  OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC



**CURB RAMP PEDESTRIAN TRAFFIC CONTROL
SIDEWALK ON SINGLE SIDE**

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

IF PEDESTRIAN PUSH BUTTONS ARE PRESENT ON THE EXISTING FACILITY, ENSURE THEY ARE MAINTAINED/ACCESSIBLE FOR PEDESTRIAN USE THROUGHOUT THE TEMPORARY PEDESTRIAN ACCOMMODATIONS.











SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

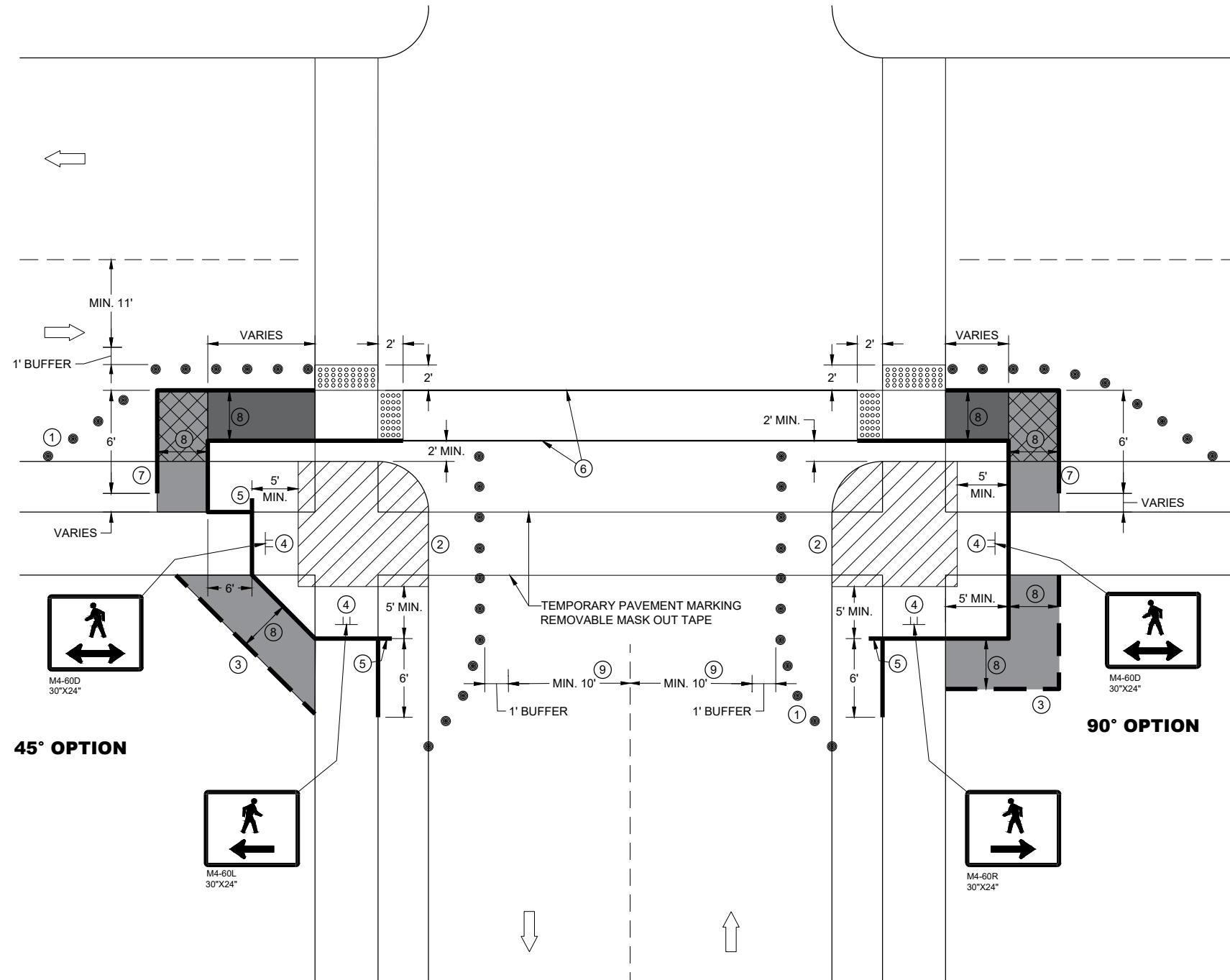
TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG

WHEN TEMPORARY PEDESTRIAN BARRICADE RUNS PARALLEL ALONG THE SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND PROPER BUFFER SPACE REQUIRED.
- ② PROVIDE ADEQUATE SPACE FOR CONTRACTOR OPERATIONS
- ③ USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
- ④ MOUNTING HEIGHT OF 5 FEET FROM SIDEWALK SURFACE TO BOTTOM OF SIGN.
- ⑤ PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL IN THE SIDEWALK TERRACE.
- ⑥ WHITE 6" TEMPORARY PAVEMENT MARKING
- ⑦ IF TEMPORARY PEDESTRIAN BARRICADE DOES NOT REACH THE FACE OF THE CURB, USE AN ADDITIONAL PANEL AND EXTEND INTO THE TERRACE.
- ⑧ 4 FEET MINIMUM, 5 FEET DESIRABLE
- ⑨ IF MINIMUM LANE WIDTHS CAN'T BE ATTAINED, CURB RAMPS MAY NEED TO BE CONSTRUCTED AT SEPARATE TIMES.

LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL DRUM
-  WORK AREA
-  TEMPORARY CURB RAMP
-  TEMPORARY PEDESTRIAN SURFACE "A"
-  TEMPORARY PEDESTRIAN SURFACE "B"
-  TEMPORARY DETECTABLE WARNING FIELD
-  TEMPORARY PEDESTRIAN BARRICADE
-  OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC









CURB RAMP PEDESTRIAN TRAFFIC CONTROL

**TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION**

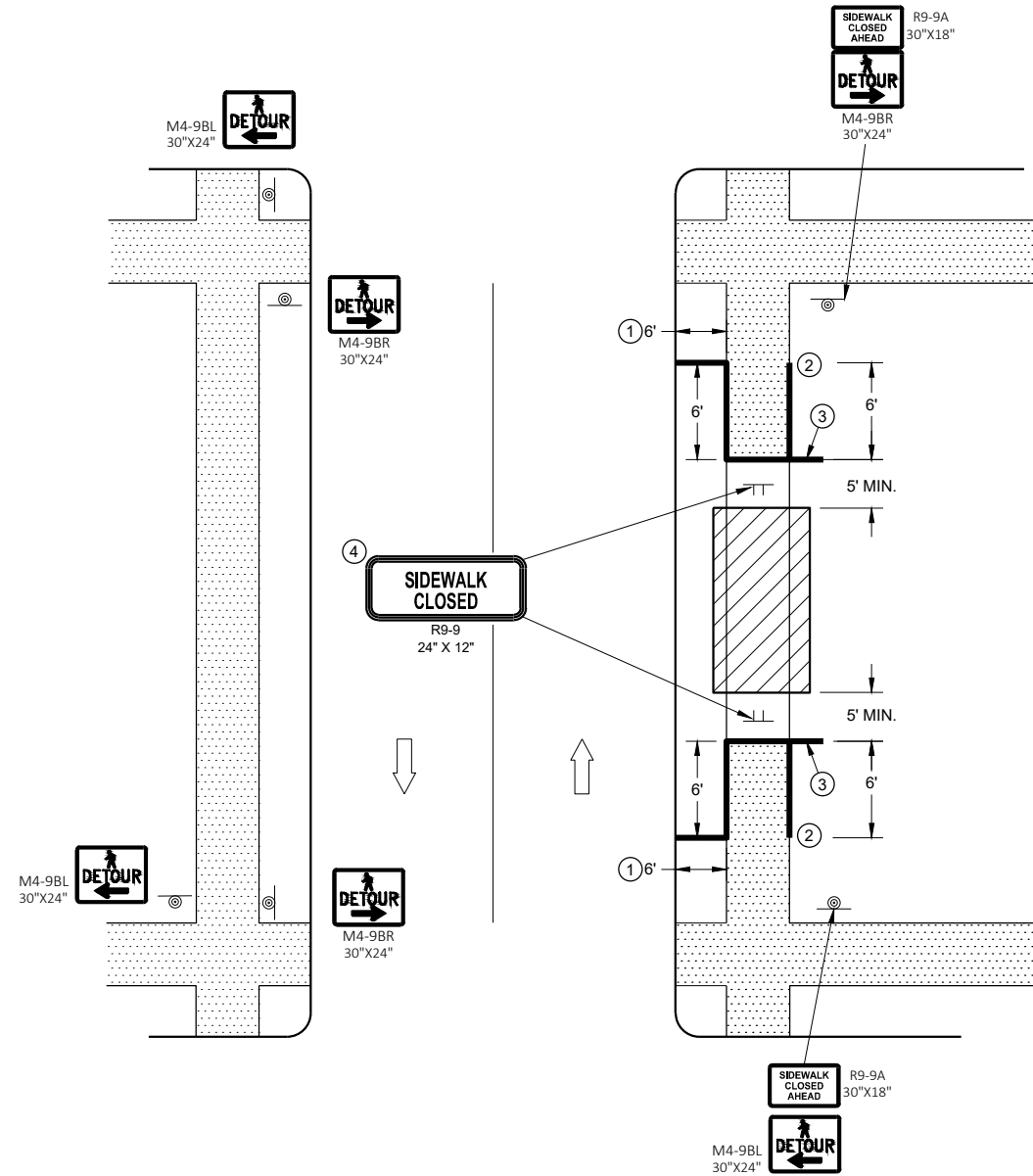
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  SIGN ON TEMPORARY SUPPORT
-  UNDER PEDESTRIAN TRAFFIC
-  WORK AREA
-  TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC

GENERAL NOTES

- SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.
- WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.
- SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.
- PLACE TEMPORARY PEDESTRIAN BARRICADE TO FIT FIELD CONDITIONS, AVOIDING CONFLICT WITH DRIVEWAYS AND OTHER EXISTING FEATURES.
- ① IF TERRACE IS LESS THAN 6 FEET WIDE, OMIT TEMPORARY PEDESTRIAN BARRICADE FROM THE SIDEWALK TO THE CURB.
 - ② PLACE BARRICADE CLOSURE SO THAT THE TEMPORARY PEDESTRIAN BARRICADE END IS AT THE LAST OPEN SIDEWALK ACCESS TO RESIDENCES OR BUSINESSES BEFORE THE SIDEWALK CLOSURE.
 - ③ IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
 - ④ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.



SIDEWALK DETOUR, SIDEWALK ON BOTH SIDES

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

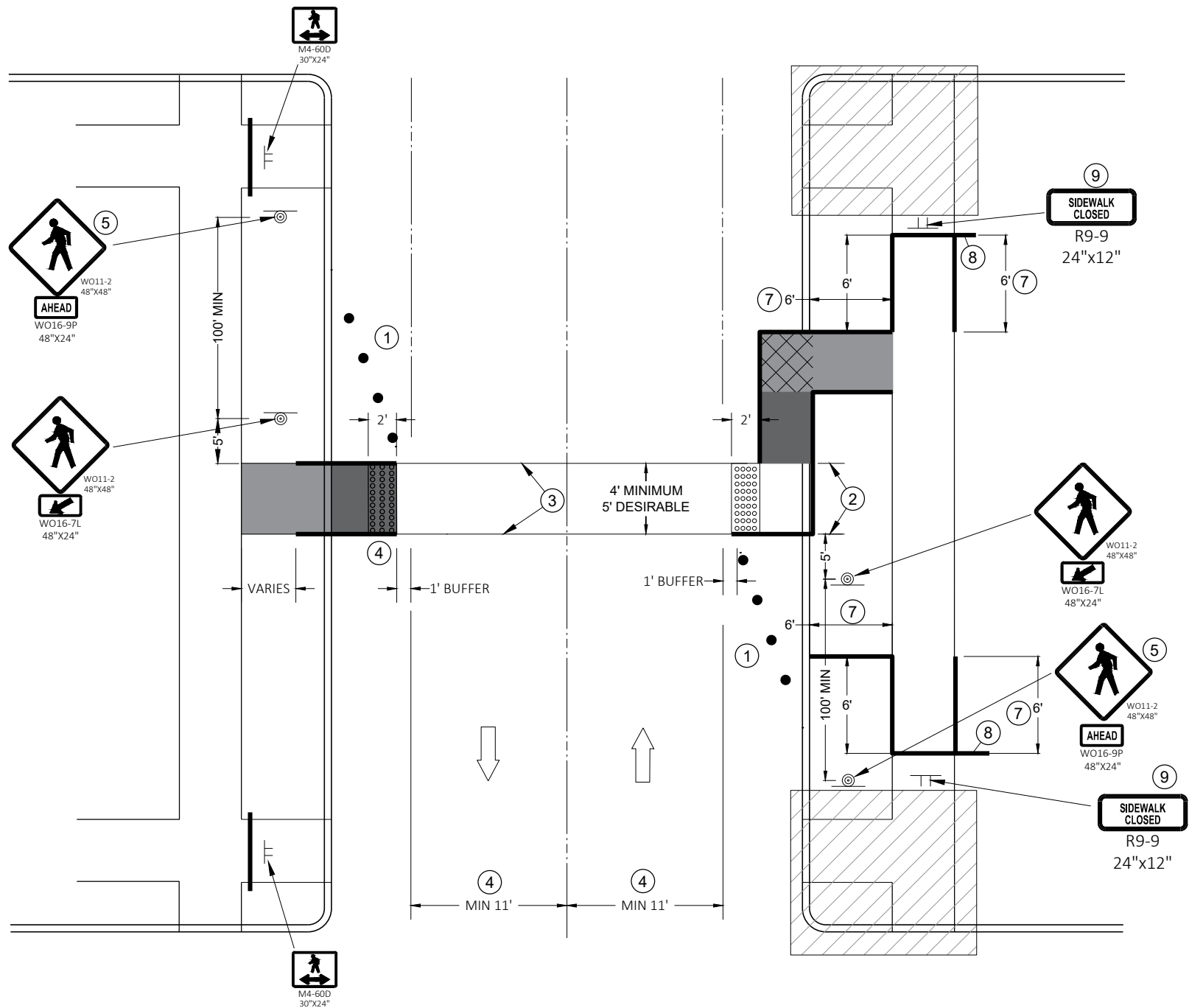
GENERAL NOTES

TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.
SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.
SEE OTHER PEDESTRIAN ACCOMMODATION DETAILS FOR SIGNING AND DEVICES FOR DIFFERENT PEDESTRIAN FACILITIES CLOSURES.

- ① SHOULDER OR LANE CLOSURE ADVANCED WARNING AND PROPER BUFFER SPACE REQUIRED.
- ② 4 FEET MINIMUM, 5 FEET DESIRABLE.
- ③ WHITE 6" TEMPORARY PAVEMENT MARKING.
- ④ IF MINIMUM LANE WIDTHS CAN'T BE ATTAINED, PERPENDICULAR CURB RAMPS MAY NEED TO BE UTILIZED.
- ⑤ IF MINIMUM 100' SPACING FROM THE MID-BLOCK CROSSING CANNOT BE ATTAINED BEFORE THE INTERSECTION, REMOVE THIS SIGN ASSEMBLY.
- ⑥ IF TERRACE IS LESS THAN 6 FEET WIDE, OMIT TEMPORARY PEDESTRIAN BARRICADE FROM THE SIDEWALK TO THE CURB.
- ⑦ PLACE BARRICADE CLOSURE SO THAT THE TEMPORARY PEDESTRIAN BARRICADE END IS AT THE LAST OPEN SIDEWALK ACCESS TO RESIDENCES OR BUSINESSES BEFORE THE SIDEWALK CLOSURE.
- ⑧ IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF THE EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
- ⑨ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF THE SIGN.

LEGEND

●	TRAFFIC CONTROL DRUM
⊥	SIGN ON TEMPORARY SUPPORT
▒	TEMPORARY CURB RAMP
◻	TEMPORARY DETECTABLE WARNING FIELD
■	TEMPORARY PEDESTRIAN SURFACE "A"
▣	TEMPORARY PEDESTRIAN SURFACE "B"
▨	WORK AREA
—	TEMPORARY PEDESTRIAN BARRICADE
➡	DIRECTION OF TRAFFIC









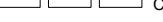


TEMPORARY PEDESTRIAN CROSSING

**TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

LEGEND

-  TYPE III BARRICADE WITH ATTACHED SIGN
-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
-  TYPE "A" WARNING LIGHT (FLASHING)
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  REMOVING PAVEMENT MARKING (SEE GENERAL NOTES)
-  CONCRETE BARRIER TEMPORARY PRECAST

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET, (500 FEET DESIRABLE) DISTANCE TO EXISTING SIGNS.

THIS LANE CLOSURE IS TYPICAL FOR LANE SHIFT RIGHT - REVERSE FOR SHIFTING LEFT.

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON ANY "COVERED" OR "DOWNED" SIGNS.

CONSIDER GEOMETRICS WHEN LOCATING SIGNS AND ARROW BOARDS SO THE DRIVER HAS A CLEAR VIEW OF THE ARROW BOARD AND LANE CLOSURE DRUMS FOR A MINIMUM 1500 FEET IN FRONT OF DRUMS.

FOR A LANE CLOSURE THAT IS IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS, THE ADVANCED WARNING SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS.

REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING LINES IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

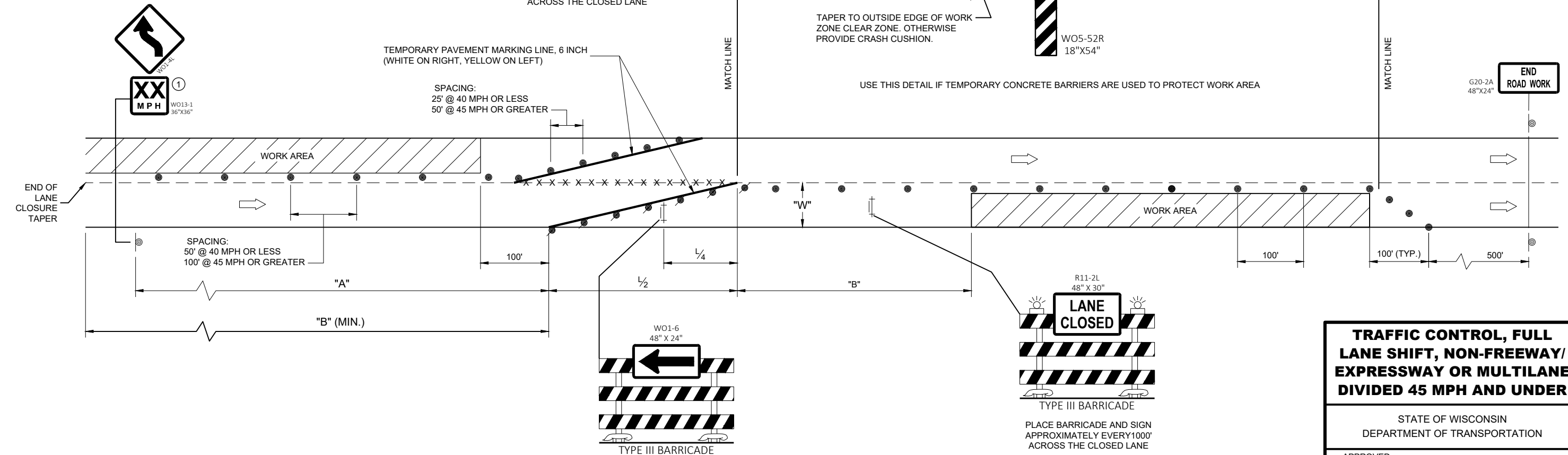
IF THE HORIZONTAL ALIGNMENT IS SUCH THAT A CURVE MAY REQUIRE ADDITIONAL DELINEATION, THE DEVICE SPACING MAY BE DECREASED TO 50 FEET.

ADJUSTMENTS IN BUFFER SPACE NEED TO BE INCORPORATED WHEN THE LANE SHIFT OCCURS NEAR AN INTERCHANGE EXIT OR ENTRANCE RAMP. THE LANE SHIFT MUST TAKE PLACE FAR ENOUGH IN ADVANCE OF AN EXIT OR ENTRANCE RAMP TO STILL ALLOW FOR ADEQUATE BUFFER SPACE. THE MINIMUM LENGTH OF THE BUFFER SPACE BEFORE AN EXIT RAMP SHOULD BE 1/2 THE LENGTH OF THE TRANSITION AREA. THE ENTRANCE RAMP SHOULD BE FOLLOWED BY THE ORIGINAL BUFFER SPACE LENGTH OF 800 FEET DESIRABLE PRIOR TO ANOTHER TRAFFIC CONTROL CHANGE SUCH AS A CROSSOVER MANEUVER.

① USE ONLY IF DESIGN SPEED IS 10 MPH BELOW POSTED SPEED.

② BARRIER FLARE RATE: 6:1 @ 40 MPH OR LESS
8:1 @ 45 MPH OR GREATER

POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET	SHIFTING TAPER $\frac{L}{2}$					BUFFER SPACE (B) FEET
		W, LATERAL OFFSET (FT)	10	11	12	13	
25	200	52	57	63	68	73	55
30	200	75	83	90	98	105	85
35	350	102	112	123	133	143	120
40	350	133	147	160	173	187	170
45	500	225	248	270	293	315	220



TRAFFIC CONTROL, FULL LANE SHIFT, NON-FREEWAY/ EXPRESSWAY OR MULTILANE DIVIDED 45 MPH AND UNDER

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED _____ /S/ Andrew Heidtke
DATE May 2023 WORK ZONE ENGINEER

FHWA








6

6

SDD 15D40-05a

SDD 15D40-05a

LEGEND

-  EXISTING SIGN ON PERMANENT SUPPORT
-  NEW SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  TYPE "A" WARNING LIGHT (FLASHING)
-  DIRECTION OF TRAFFIC
-  WORK ZONE

GENERAL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

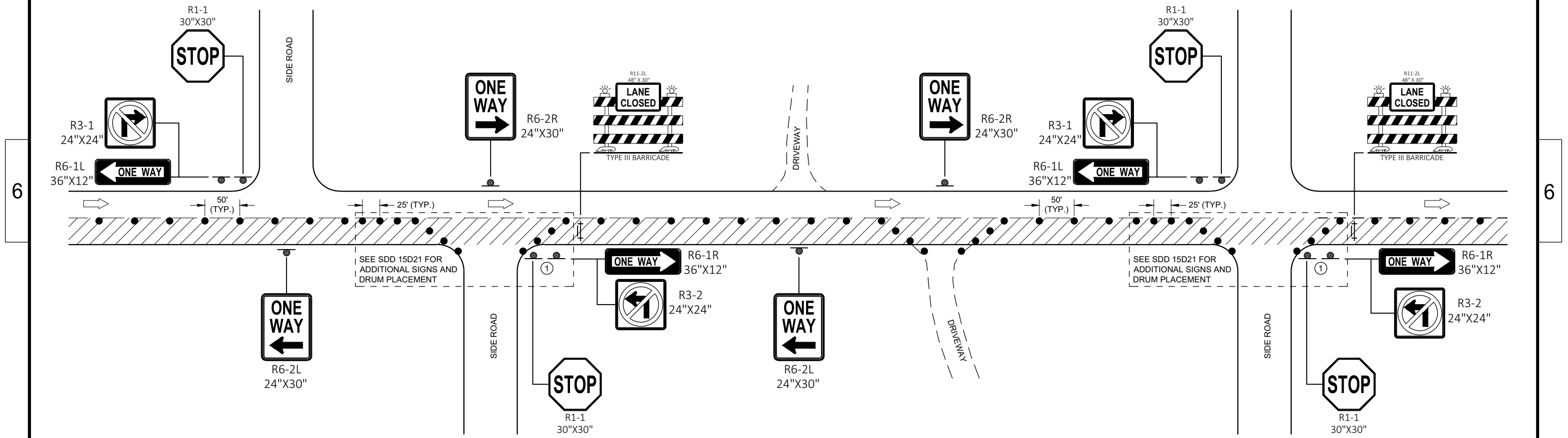
ALL SIGNS AND DEVICES SHALL BE IN CONFORMANCE WITH THE WISCONSIN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD). SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISDOT STANDARD SIGN PLATES.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON COVERED OR "DOWNED" SIGNS.

BARRICADES IN A CLOSED LANE THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.

① IF WORK OPERATIONS ALLOWS, MOVE R1-1, R6-1R, AND R3-2 CLOSER TO INTERSECTION.



**TRAFFIC CONTROL
ONE - WAY SIGNING**

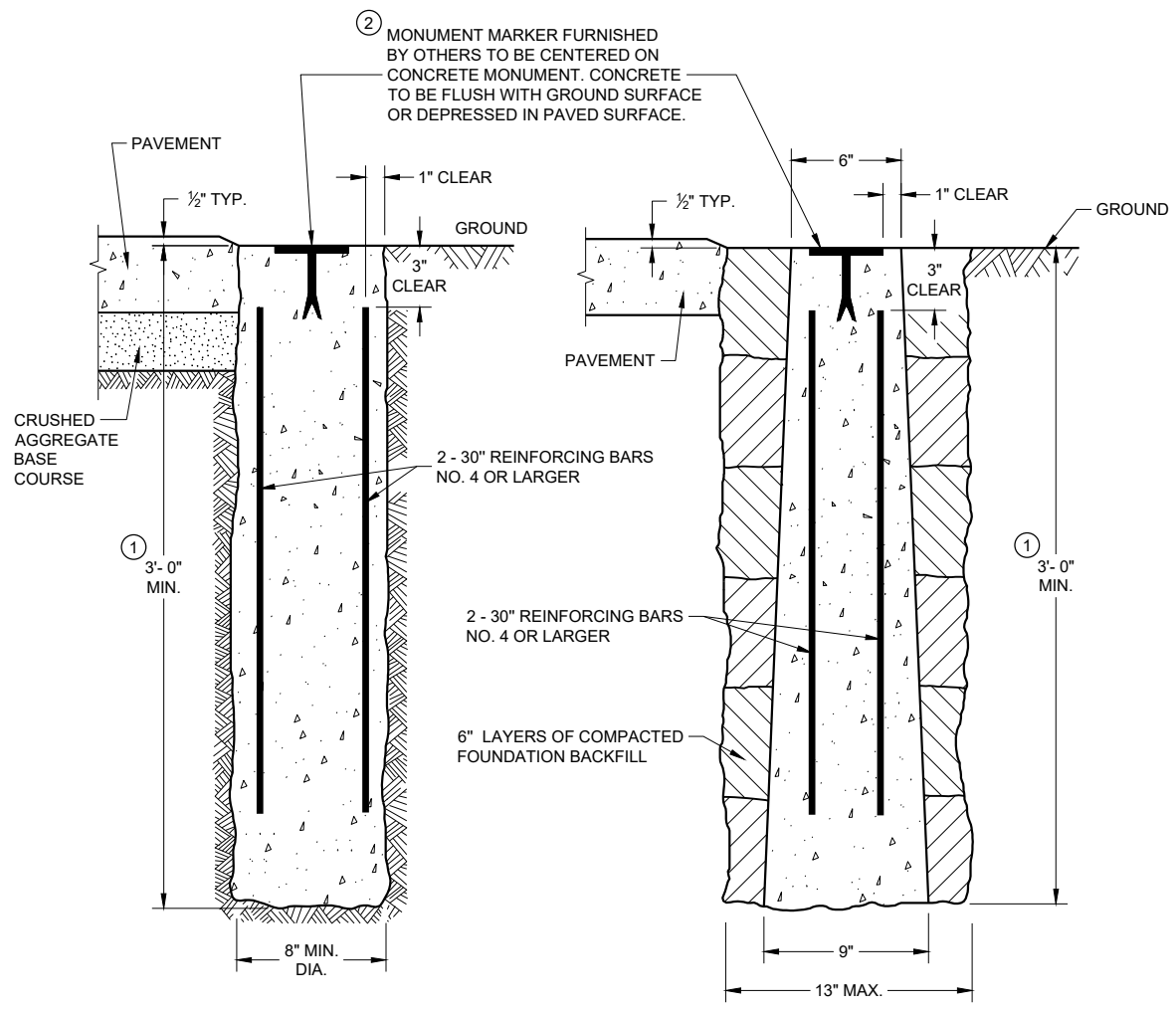
**TRAFFIC CONTROL
ONE-WAY SIGNING**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

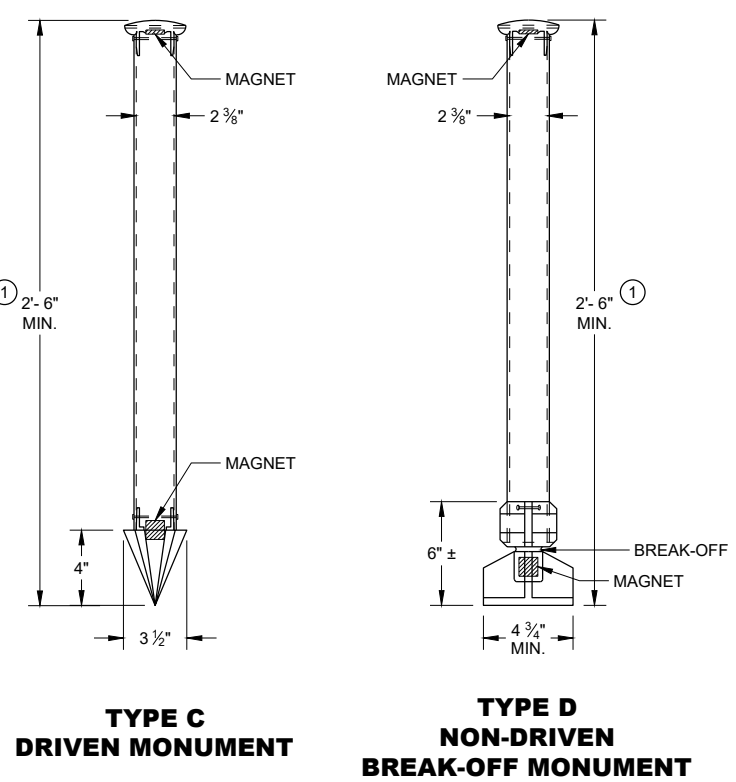
APPROVED
DATE May 2020 /S/ Andrew Heidtke
WORK ZONE ENGINEER

SDD 15D46 - 01

SDD 15D46 - 01



**CAST-IN-PLACE
PRECAST
CONCRETE MONUMENTS
TYPE A**



**TYPE C
DRIVEN MONUMENT**
**TYPE D
NON-DRIVEN
BREAK-OFF MONUMENT**
**ALUMINUM MONUMENTS
(INCLUDES MARKER)**

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

DETAILED DRAWINGS OF PROPOSED ALTERNATE DESIGNS FOR METAL MONUMENTS OR MONUMENT COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.

PERMANENT MAGNETS SHALL BE INSERTED NEAR THE TOP AND BOTTOM OF ALL ALUMINUM MONUMENTS SO THE MONUMENT CAN EASILY BE DETECTED BY A METAL DETECTOR.

THE CAST IRON MONUMENT COVER SHALL BE A "NON-ROCKING" TYPE. ADJUSTMENT OF THE COVER TO GRADE MAY BE ACCOMPLISHED BY THE USE OF MORTAR AND BRICK, OR BY EITHER PRECAST OR CAST-IN-PLACE REINFORCED CONCRETE GRADE RINGS.

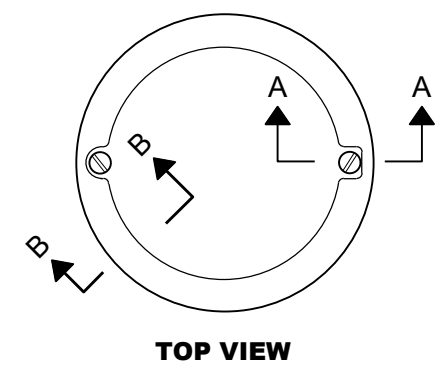
MONUMENTS SHALL BE LOCATED AND PLACED AT THE DIRECTION OF THE ENGINEER.

ALUMINUM MONUMENTS AND MONUMENT COVERS SHALL BE MADE FROM AN ALUMINUM AND MAGNESIUM ALLOY AS DETERMINED BY THE MANUFACTURER.

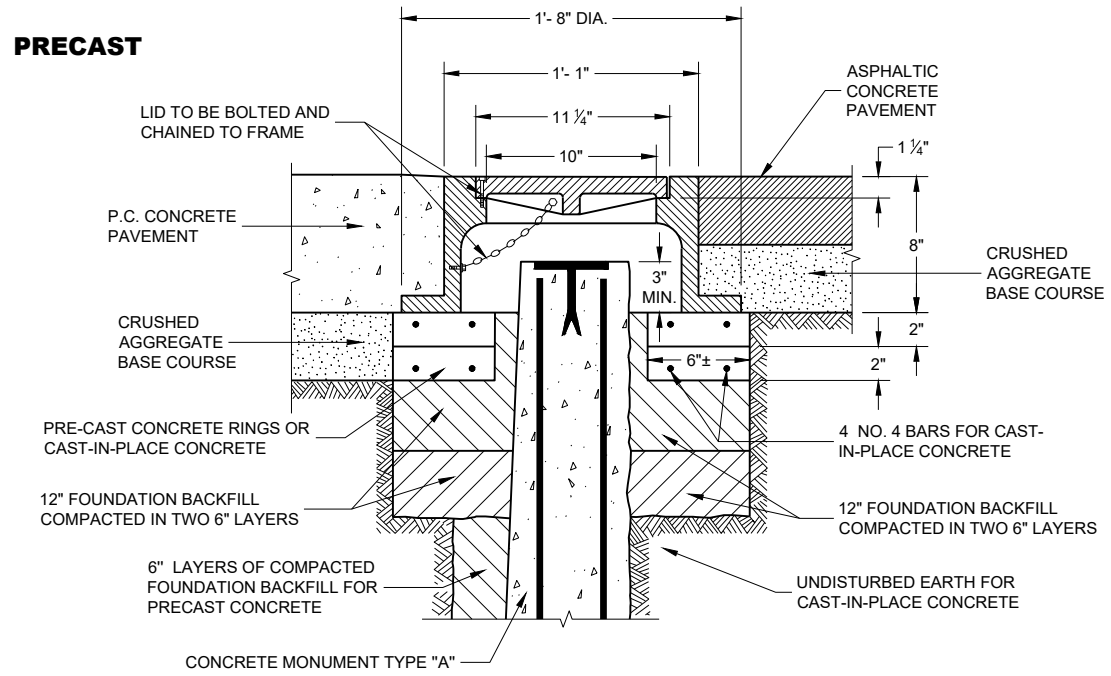
THE MONUMENT COVERS DETAILED ON THIS DRAWING ARE NOT EQUAL ALTERNATES. MONUMENT COVERS SHALL BE CAST IRON UNLESS ALUMINUM IS SPECIFIED ELSEWHERE IN THE CONTRACT.

MONUMENT SHALL BE CAST-IN-PLACE CONCRETE UNLESS PRECAST CONCRETE OR ALUMINUM MONUMENTS ARE SPECIFIED IN THE CONTRACT OR PERMITTED BY THE ENGINEER.

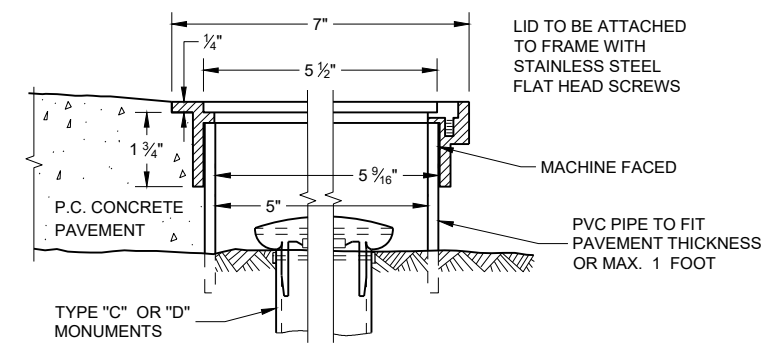
- ① MINIMUM LENGTH SHALL BE 4'-0" FOR MONUMENTS INSTALLED IN PAVED AREAS.
- ② AN OFFICIAL COUNTY MONUMENT MARKER SUPPLIED BY A COUNTY MAY BE REQUIRED FOR SOME SECTION CORNERS AND WITNESS MONUMENTS INSTEAD OF THIS WISDOT MARKER.



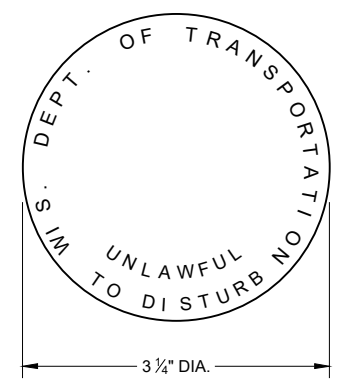
TOP VIEW



**CAST IRON MONUMENT COVER
(APPROXIMATE WEIGHT 95 LBS)**



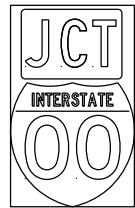
**SECTION B-B SECTION A-A
ALUMINUM MONUMENT COVER
(APPROXIMATE WEIGHT 2 LBS)
(FOR CONCRETE PAVEMENT ONLY)**



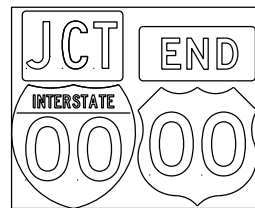
② **WIS DOT MONUMENT MARKER LOGO**
FOR TYPES "A", "C" & "D"

LANDMARK REFERENCE MONUMENTS AND COVERS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED March 2018 DATE	/s/ Raymond A. Kumapayil CHIEF SURVEYING AND MAPPING ENGINEER
FHWA	

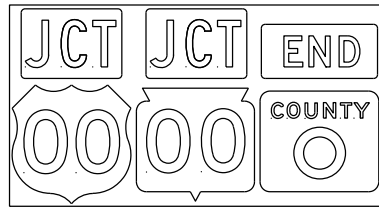
TYPICAL ASSEMBLIES



J1-1



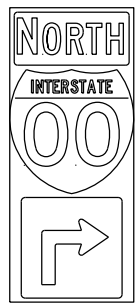
J1-2



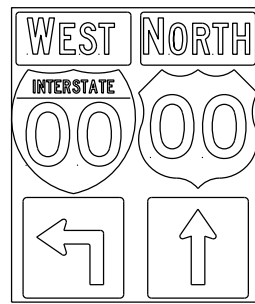
J1-3



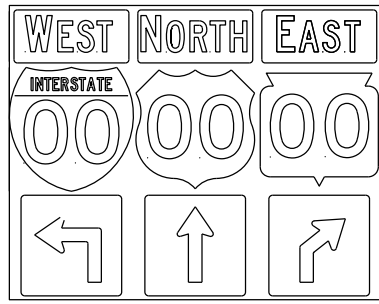
JR1-1



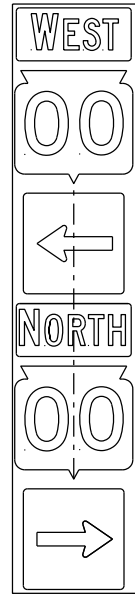
J2-1



J2-2

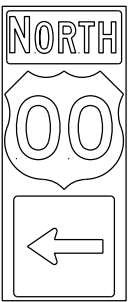


J2-3

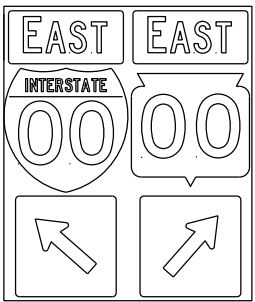


JV

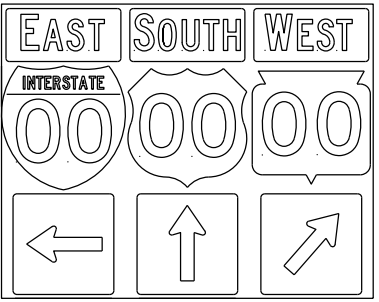
(Typical Vertical J-Assembly
See Note 10 and 11)



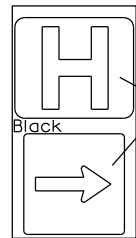
J3-1



J3-2



J3-3



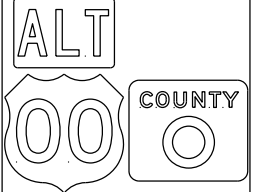
JH-1

Blue Background

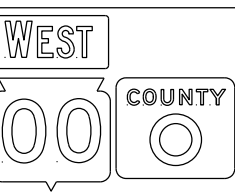
Black



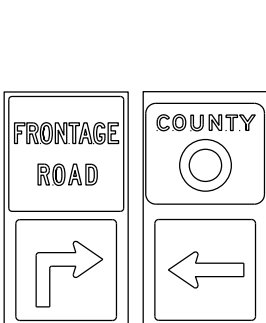
J4-1



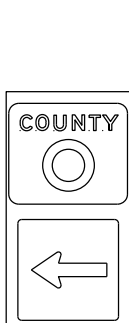
J4-2



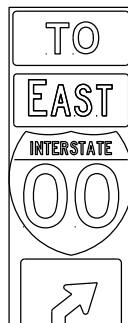
J4-2



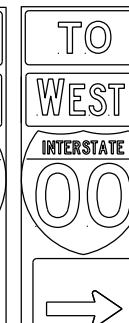
J12-1



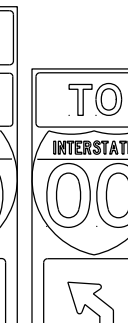
J13-1



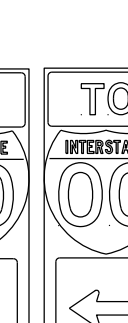
J32-1



J33-1



J22-1



J23-1



JR13-1

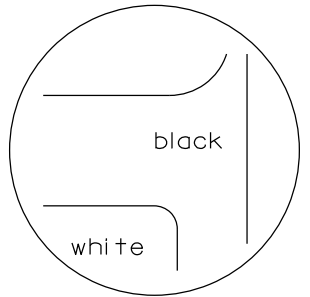
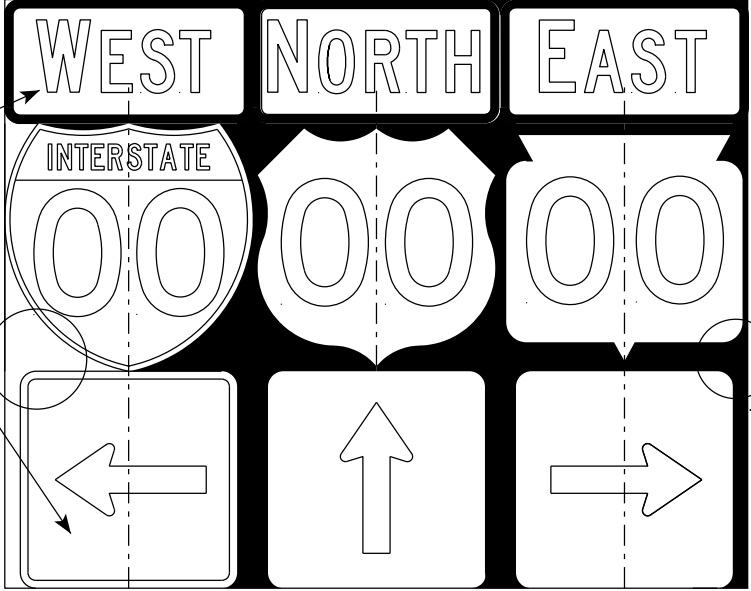
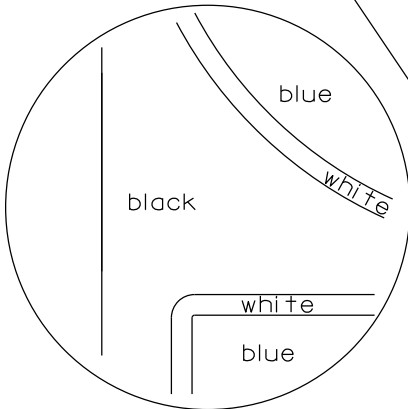


JR23-1



JR99-1

blue background with interstate



black background

ROUTE MARKERS & COMPONENTS IN TYPICAL ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Raub*
For State Traffic Engineer

DATE 6/7/23 PLATE NO. A2-1S.10

NOTES

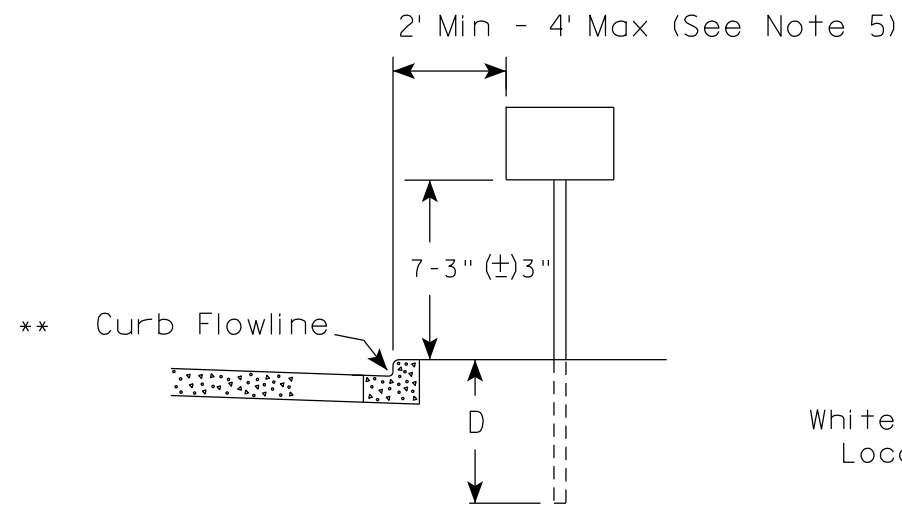
- Signs are Type II - Type H Reflective
- Color:
Background - Black Non-reflective
Message - see Note 4
- Message Series - See Note 4
- The colors and message spacing on each marker shall be according to the applicable route marker panel specifications.
- Certain marker heads require the component pieces to be the same color. As an example, all the components used with an MI-1 Interstate marker shall be blue.
- Single panel j-assemblies shall only be used with route marker shields that are same size. If the route marker shields are different size use multiple piece component.
- Route assemblies that have 24 inch route shields and have dimensions greater than 48 inches (both vertical and horizontal) shall have one horizontal splice between the arrows and route shields. Vertical splices shall not be used on route assemblies with a horizontal dimension of 144 inches or less. The contractor shall not use more than one vertical joint per sign and the joint shall be between route shields.
- Route assemblies that have 36 inch shields and have dimensions greater than 48 inches (both vertical and horizontal) shall have two horizontal splices. One horizontal splice shall be between the cardinal direction and route shields and the other horizontal splice shall be between the arrows and route shields. Vertical splices shall not be used on route assemblies with a horizontal dimension of 144 or less. The contractor shall not use more than one vertical joint per sign and the joint shall be between route shields.
- All Vertical J Assemblies are given a Sign Code of JV
- For JV Assemblies that have a mixture of Interstate and Non-Interstate shields, arrows and cardinals shall be white on blue.
- For JV Assemblies that have a mixture of Non-Interstate and Auto-Tour shields, arrows and cardinals shall be black on white.

7

7

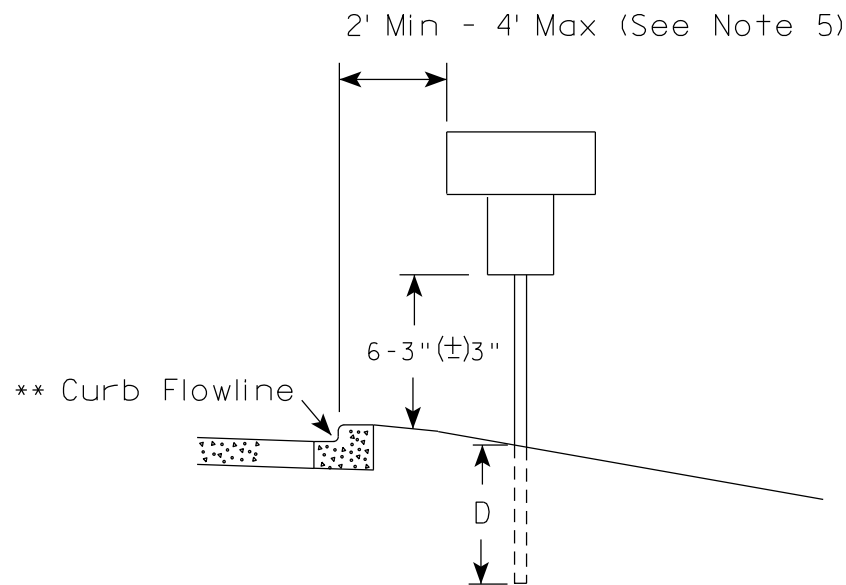
URBAN AREA

RURAL AREA (See Note 2)



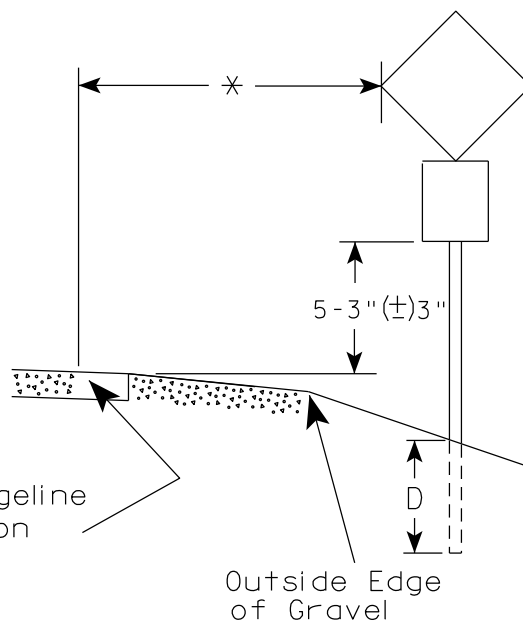
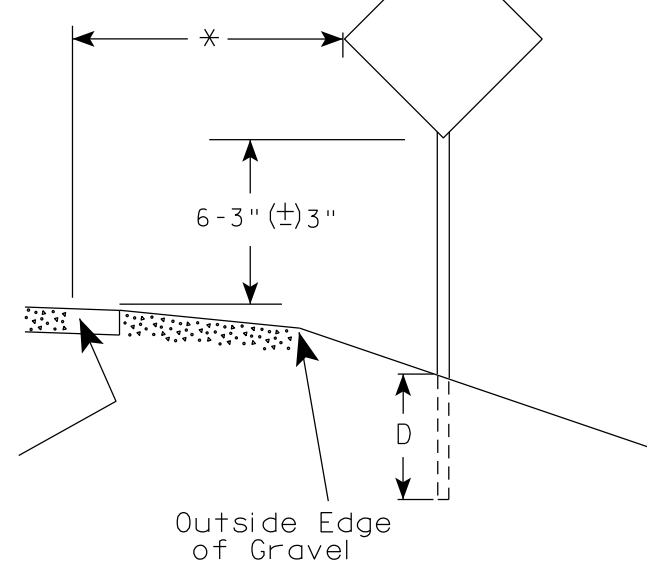
White Edgeline Location

Outside Edge of Gravel



White Edgeline Location

Outside Edge of Gravel



POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

GENERAL NOTES

1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on or behind barrier wall, see A4-10 sign plate.
The Double Arrow sign (W12-1D) shall be mounted at a height of 2'-3" (±) 3". The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±) 3".
3. For expressways and freeways, mounting height is 7'- 3" (±) 3" or 6'-3" (±) 3" depending upon existence of a sub-sign.
4. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±) 3".
5. Offset distance shall be consistent with existing signs or consistent throughout length of project.
6. Folding signs shall be mounted at a height of 5'-3" (±) 3" or as directed by the Engineer.

* * The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Raub*
for State Traffic Engineer

DATE 12/6/23 PLATE NO. A4-3.23



ELEVATION VIEW

DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

- NOTES:**
1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
 2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
 3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



PLAN VIEW

FOR NEW CONCRETE/ ASPHALT INSTALLATIONS

SIGN POST BOX-OUTS A4-3B	
<small>WISCONSIN DEPT OF TRANSPORTATION</small>	
APPROVED <i>Matthew R Rauch</i> <small>for State Traffic Engineer</small>	
<small>DATE 1/27/14</small>	<small>PLATE NO. A4-3B.1</small>

GENERAL NOTES

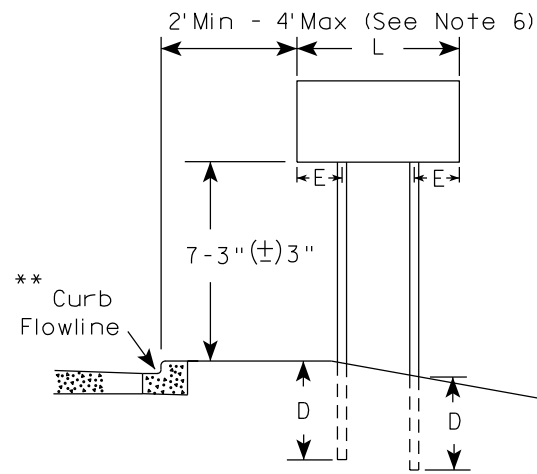
1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
2. See tables below for required number of posts.
3. For expressways and freeways, mounting height is 7'-3" (\pm 3") or 6'-3" (\pm 3") depending upon existence of sub-sign.
4. The (\pm) tolerance for mounting height is 3 inches.
5. J-Assemblies are considered to be one sign for mounting height.
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. Folding signs shall be mounted at a height of 5'-3" (\pm 3") or as directed by the engineer.
8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (\pm 3"). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (\pm 3").

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

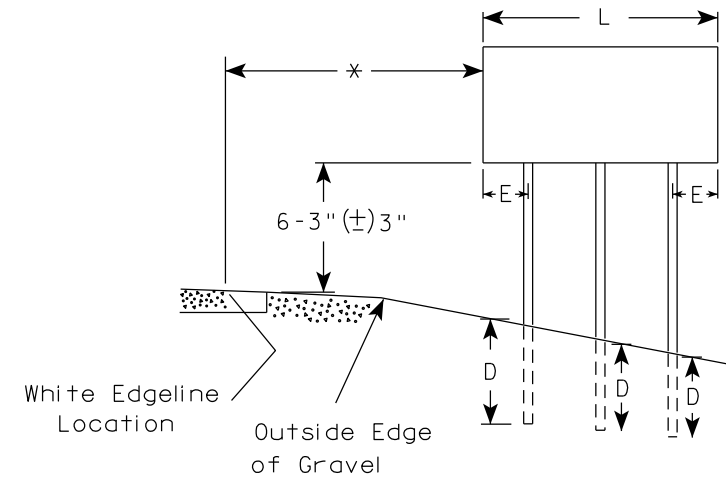
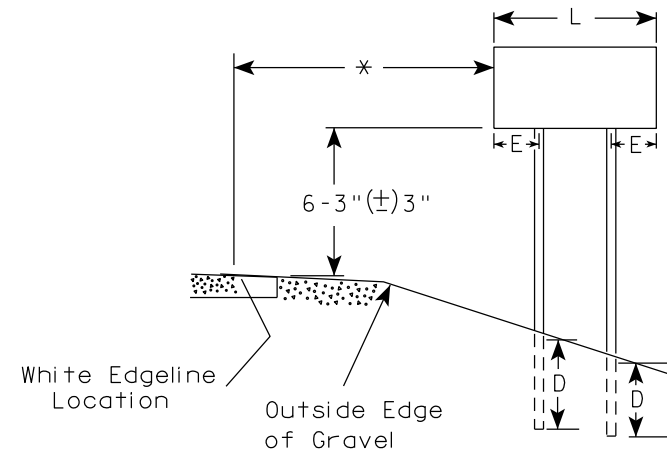
** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

*** See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

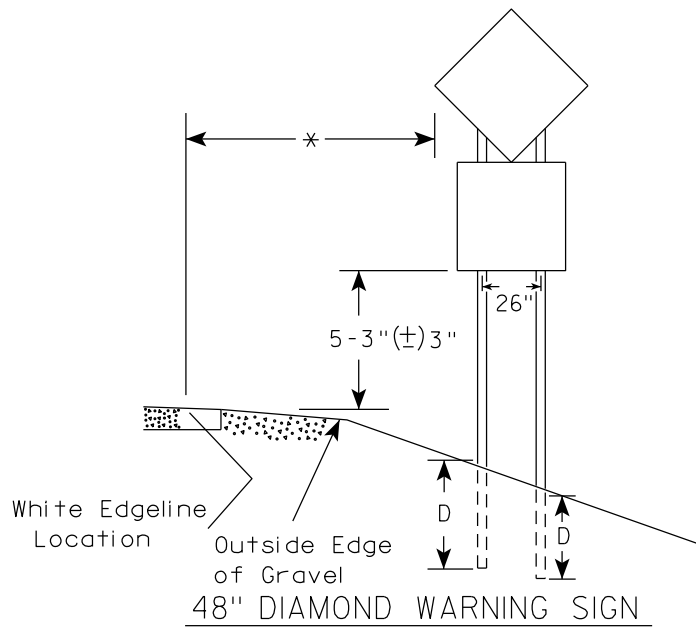
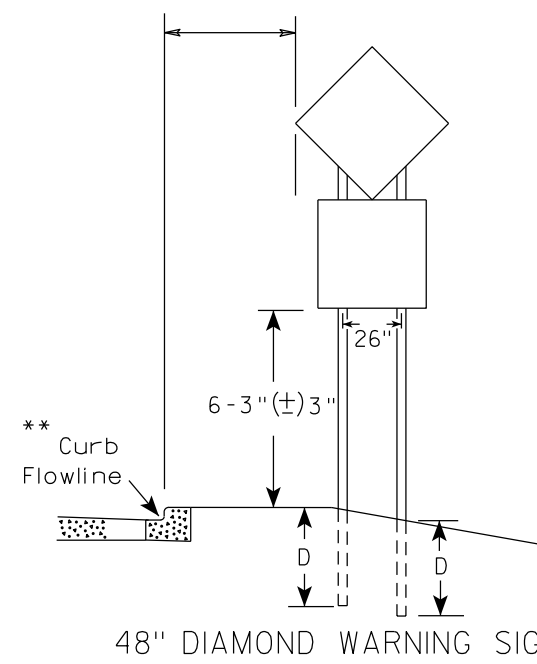
URBAN AREA



RURAL AREA (See Note 3)



URBAN AREA



SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)	
L	E
Greater than 48" Less than 60"	12"
60" to 108"	L/5

SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)	
L	E
Greater than 108" to 144"	12"

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Rauch*
For State Traffic Engineer

DATE 12/6/23 PLATE NO. A4-4.16

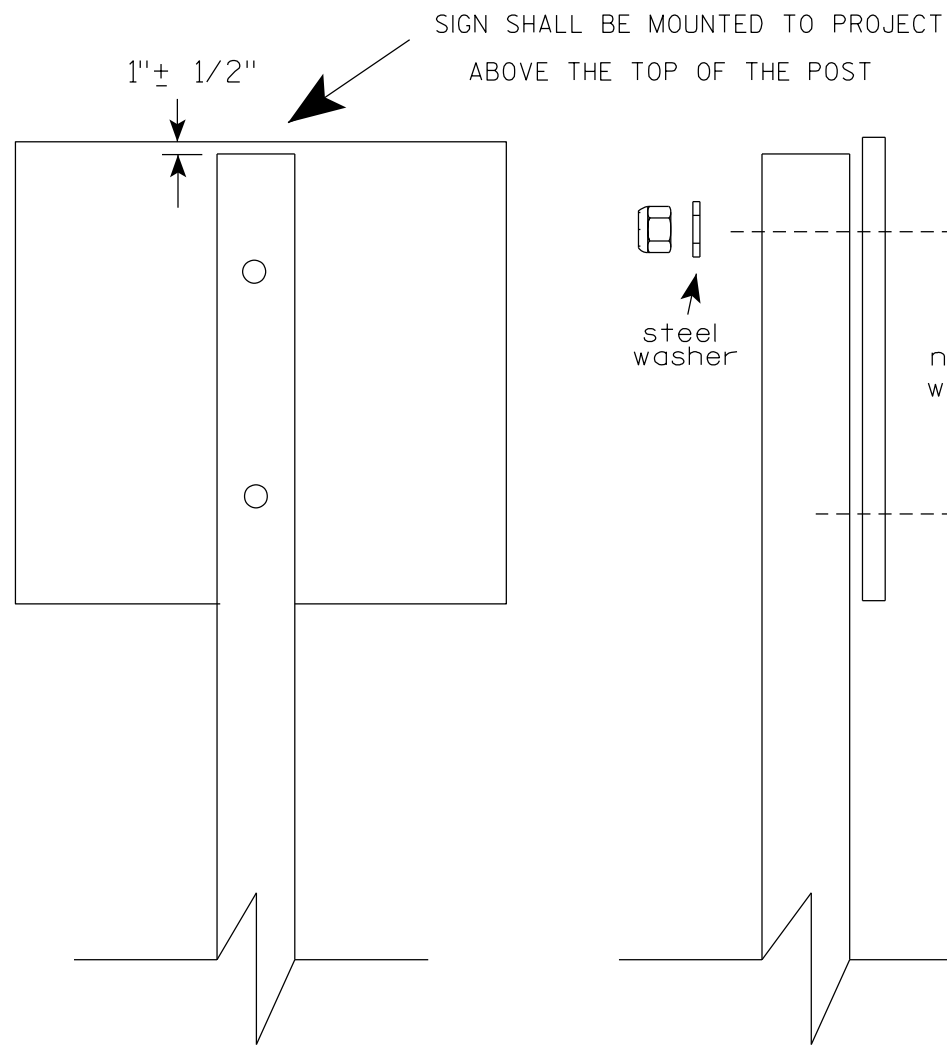
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

- Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
- Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

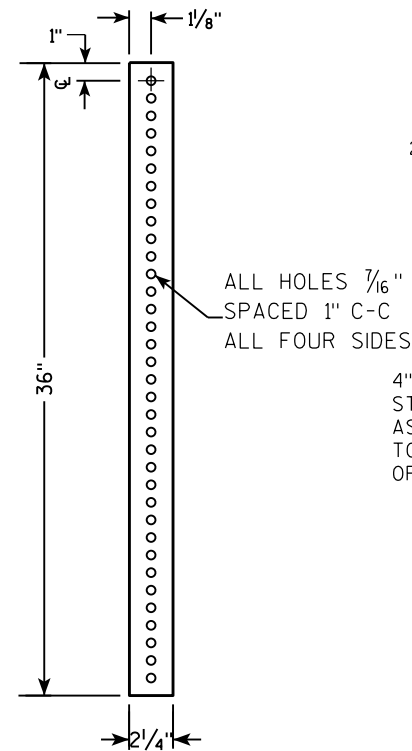
- STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)
- MACHINE BOLTS - $\frac{5}{16}$ " X 1-3/4" Length w/ lock nuts
- WOOD POSTS (4" x 6")
- LAG SCREWS - $\frac{3}{8}$ " X 3" (NO STRINGERS ON BACK OF SIGN)
 $\frac{3}{8}$ " X 4" (STRINGERS ON BACK OF SIGN)
- SQUARE STEEL POSTS (2" x 2")
- MACHINE BOLTS - $\frac{3}{8}$ " X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)
 $\frac{3}{8}$ " X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)
- RIVETS - $\frac{9}{32}$ " (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
 O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH
- WASHERS (ALL POSTS) -
- 1-1/4" O.D. X $\frac{3}{8}$ " I.D. X $\frac{1}{16}$ " STEEL
- 1-1/4" O.D. X $\frac{3}{8}$ " I.D. X .080 NYLON

* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS TO POSTS	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R Rauch</i> For State Traffic Engineer
DATE 4/1/2020	PLATE NO. A4-8.9

**TELESCOPIC TUBING ANCHORS
TWO PIECE SYSTEM**

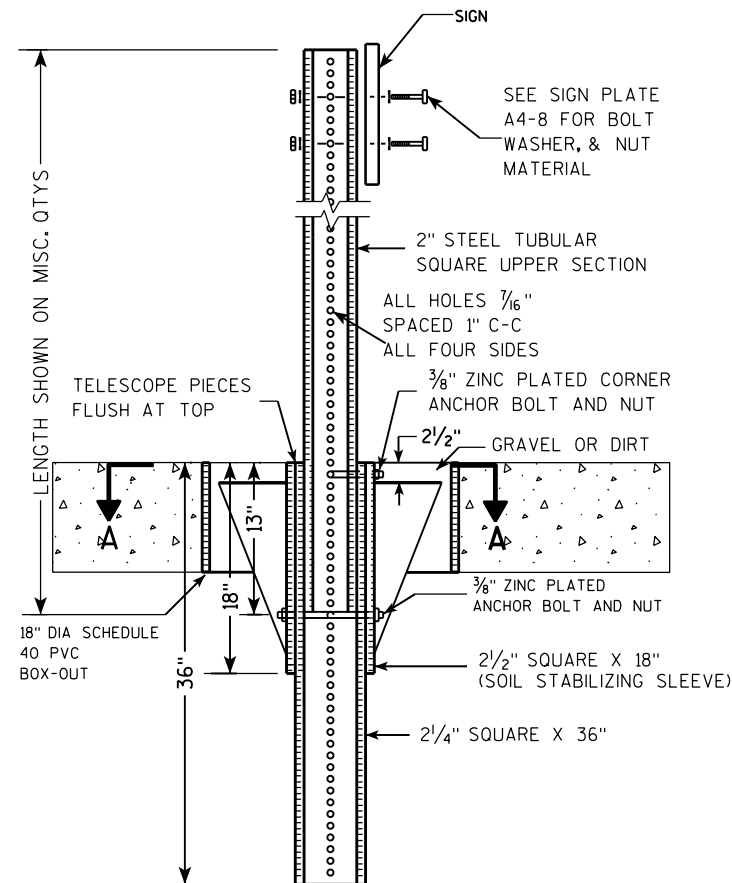
2 1/4" SQUARE
12 GAUGE
PERFORATED
GALVANIZED FINISH



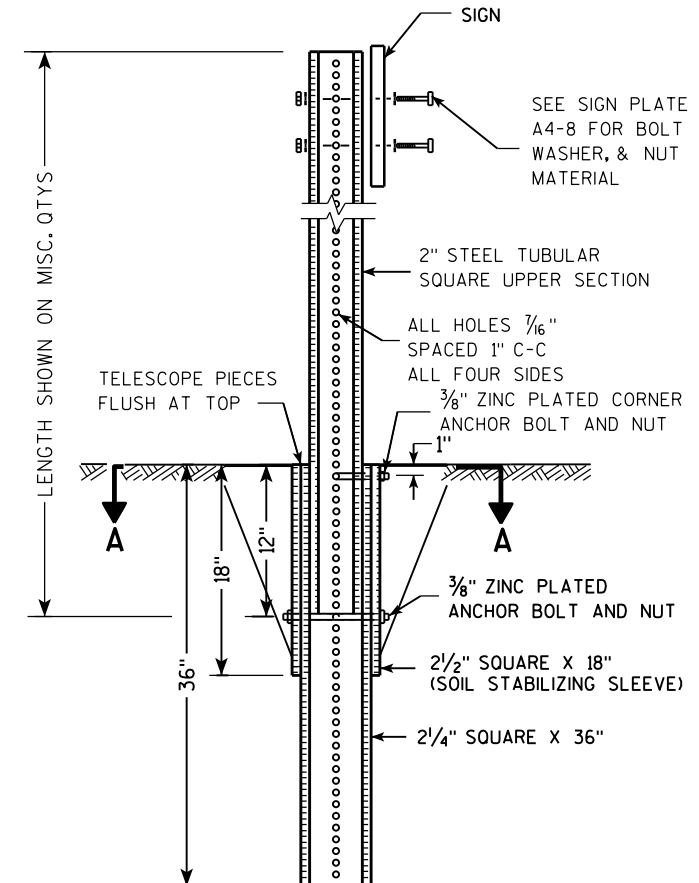
2 1/2" SQUARE
12 GAUGE
OMNI-DIRECTIONAL
PERFORATED
SOIL STABILIZING SLEEVE
GALVANIZED FINISH



**DETAIL OF TUBULAR STEEL SIGN POST
(IN POURED CONCRETE OR ASPHALT)**



**DETAIL OF TUBULAR STEEL SIGN POST
(IN LOCATIONS OTHER THAN POURED CONCRETE OR ASPHALT)**

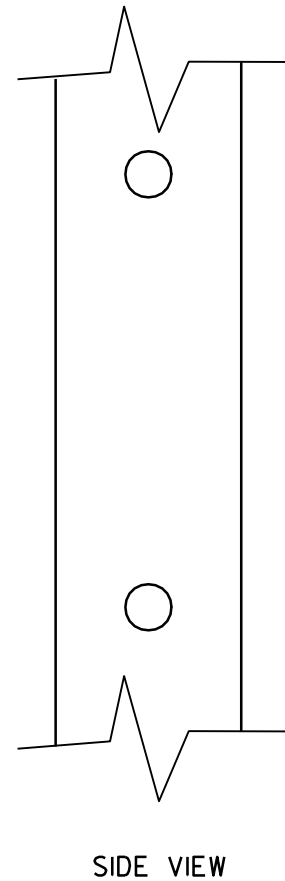
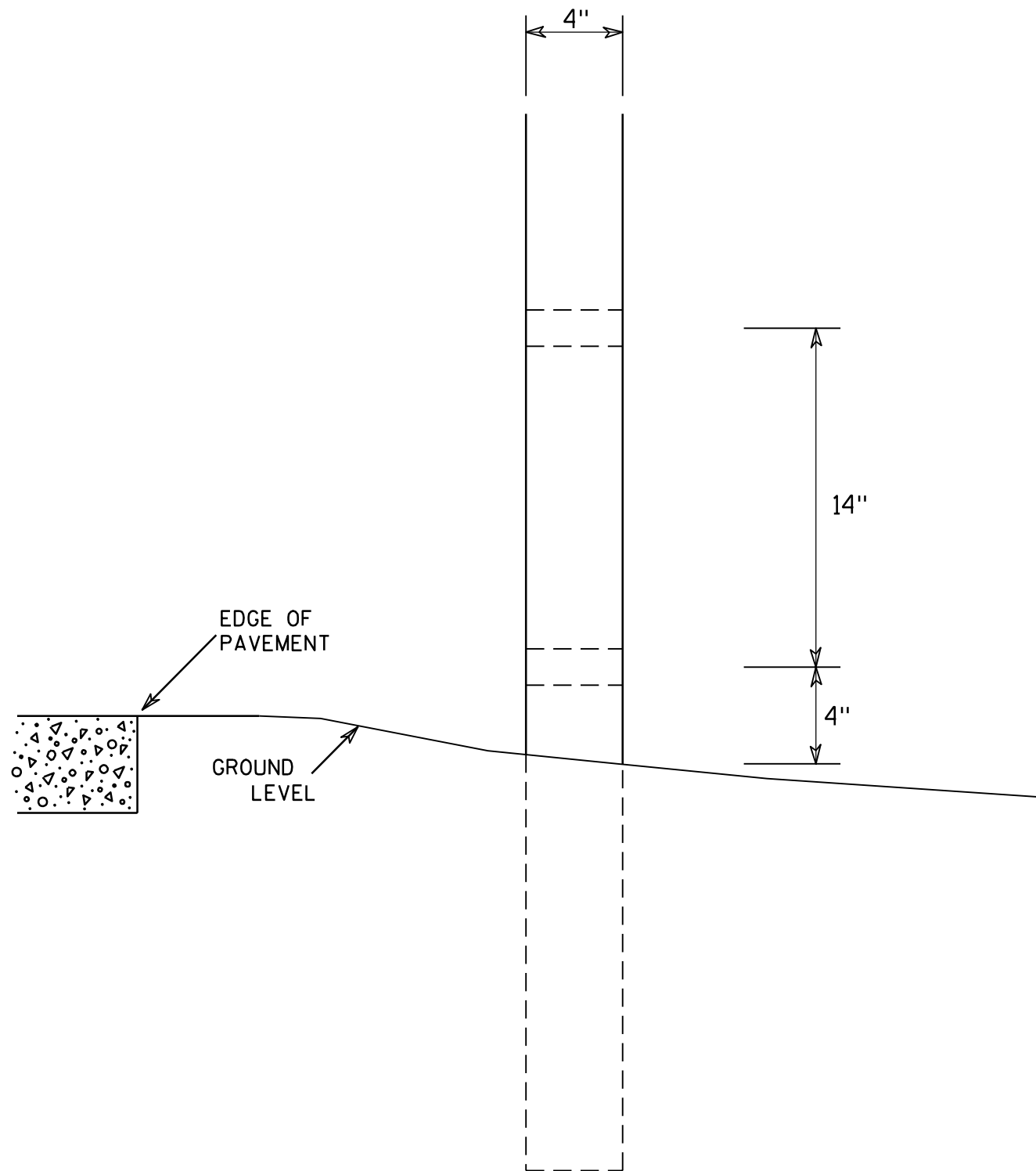


Area of Sign Installation (Sq. Ft.)	Number of Required Posts
9 or less	1
Greater than 9 less than or equal to 18	2
Greater than 18 less than or equal to 27	3

Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

**TUBULAR STEEL
SIGN POST
A4-9**

WISCONSIN DEPT OF TRANSPORTATION
APPROVED *Matthew R. Rauch*
for State Traffic Engineer
DATE 2/05/15 PLATE NO. A4-9.9



GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

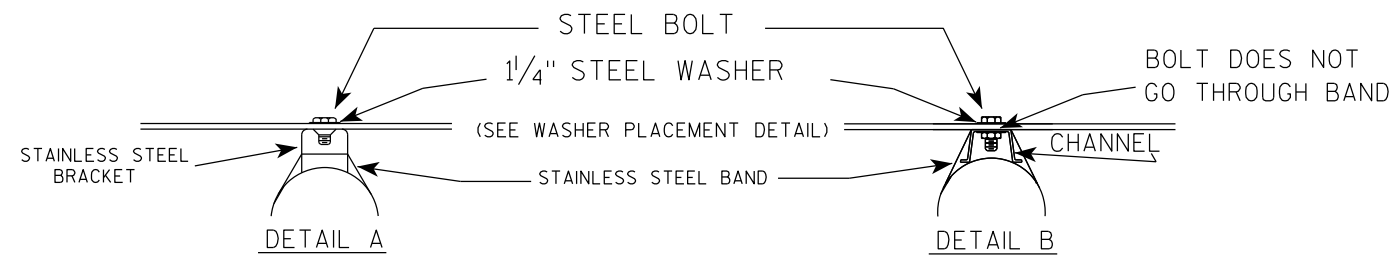
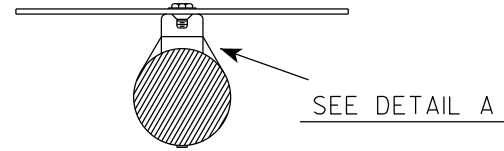
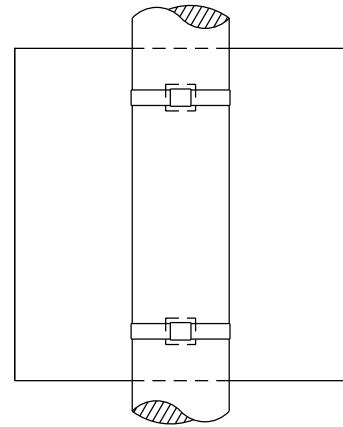
7

7

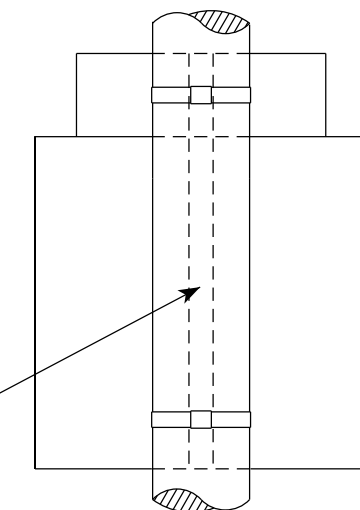
4 X 6 WOOD POST MODIFICATIONS	
<i>WISCONSIN DEPT OF TRANSPORTATION</i>	
APPROVED	<i>Chester J Spang</i> for State Traffic Engineer
DATE <u>3/27/97</u>	PLATE NO. <u>A4-11.2</u>

BANDING

SINGLE SIGN



"J" ASSEMBLY

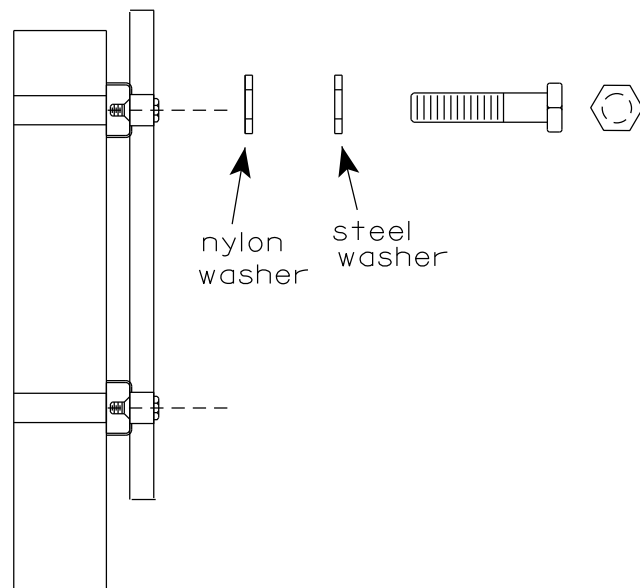


CHANNEL
SEE TYPICAL PANEL
INSTALLATION SHEET



- GENERAL NOTES**
1. Any sign over 3 feet in width shall use the V-Block banding method. See A5-10 standard plate.
 2. Signs 3 feet or greater in height shall have three bracket bands installed. Signs less than 3 feet in height shall have two bracket bands installed.
 3. Banding and assembly bracket shall be stainless steel. All bands shall be 3/4" in width and 0.025" thickness.
 4. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
 - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
 - b. Electro-galvanized in accordance with ASTM designation: B 633, Type III, SC 3

WASHER PLACEMENT



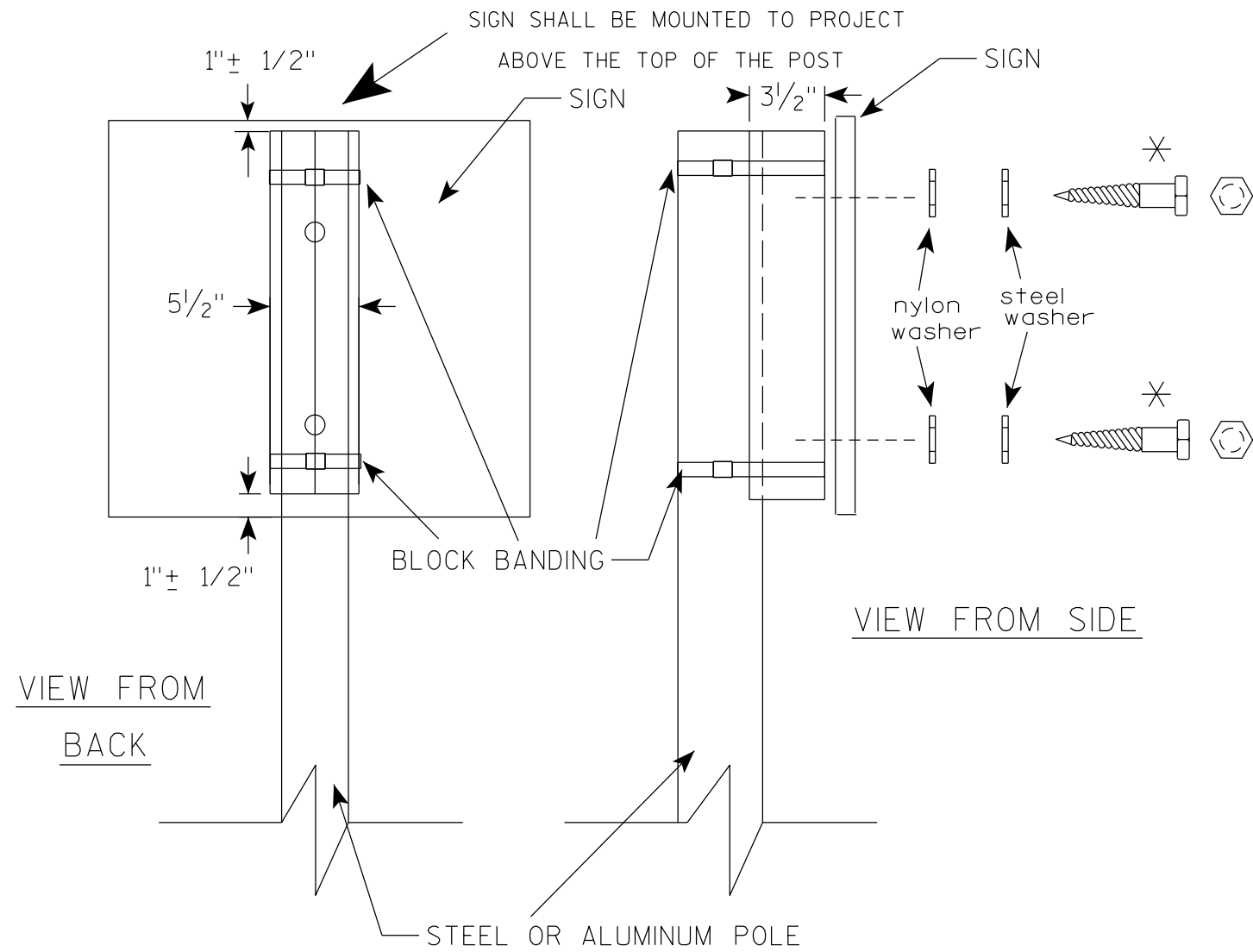
WASHERS (ALL POSTS) -
1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
1-1/4" O.D. X 3/8" I.D. X .080 NYLON
FOR ALL TYPE H SIGNS

STANDARD SIGN
SIGN BANDING DETAILS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

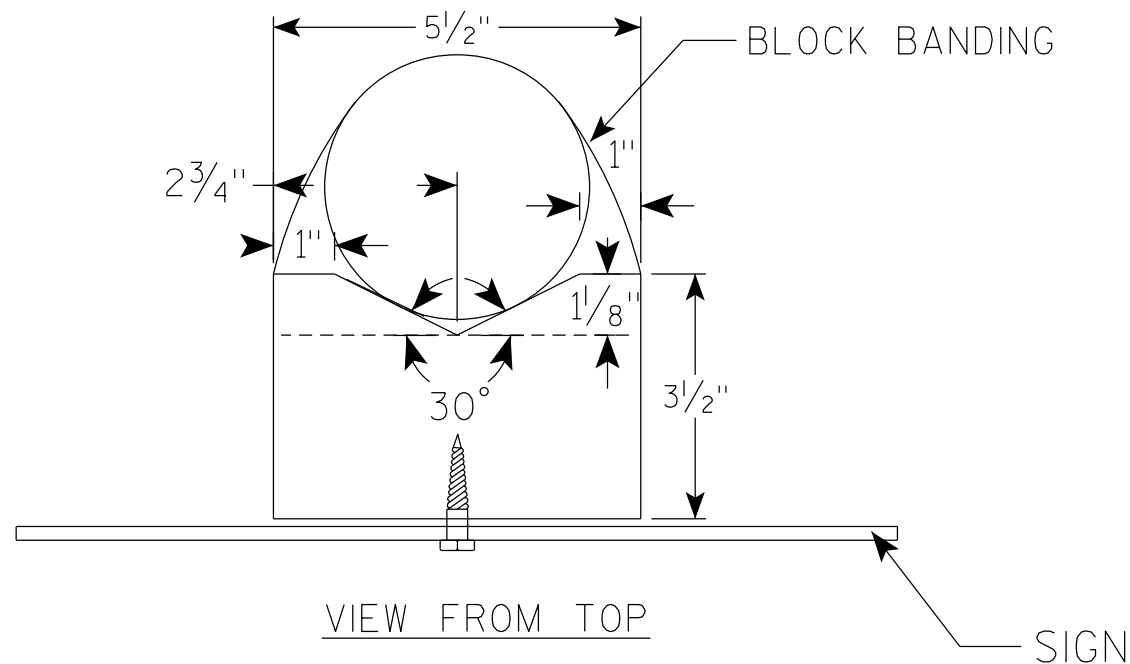
DATE 6/10/19 PLATE NO. A5-9.4



GENERAL NOTES

1. WOOD 4"X6" POST MATERIAL SHALL CONFORM TO 507.2.2 OF THE WISDOT STANDARD SPECIFICATIONS
2. BLOCK BANDING AND CLIPS SHALL BE STAINLESS STEEL, 3/4" WIDTH AND 0.025" THICKNESS
3. SIGNS 3' OR GREATER IN HEIGHT SHALL UTILIZE 3 BLOCK BANDS. SIGNS UNDER 3' IN HEIGHT SHALL UTILIZE 2 BLOCK BANDS
4. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA, BUT NORMALLY THERE ARE TWO. FOR SIGNS GREATER THAN 9 S.F. 3 FASTENERS SHALL BE USED.
5. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
 - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
 - b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3
6. ALL BOLTS SHALL HAVE HEXAGONAL HEADS.
7. STEEL WASHERS SHALL BE 1/4" O.D. X 3/8" I.D. X 1/16"
8. NYLON WASHERS SHALL BE 1/4" O.D. X 3/8" I.D. X .080 FOR TYPE H OR TYPE F FACE SIGN

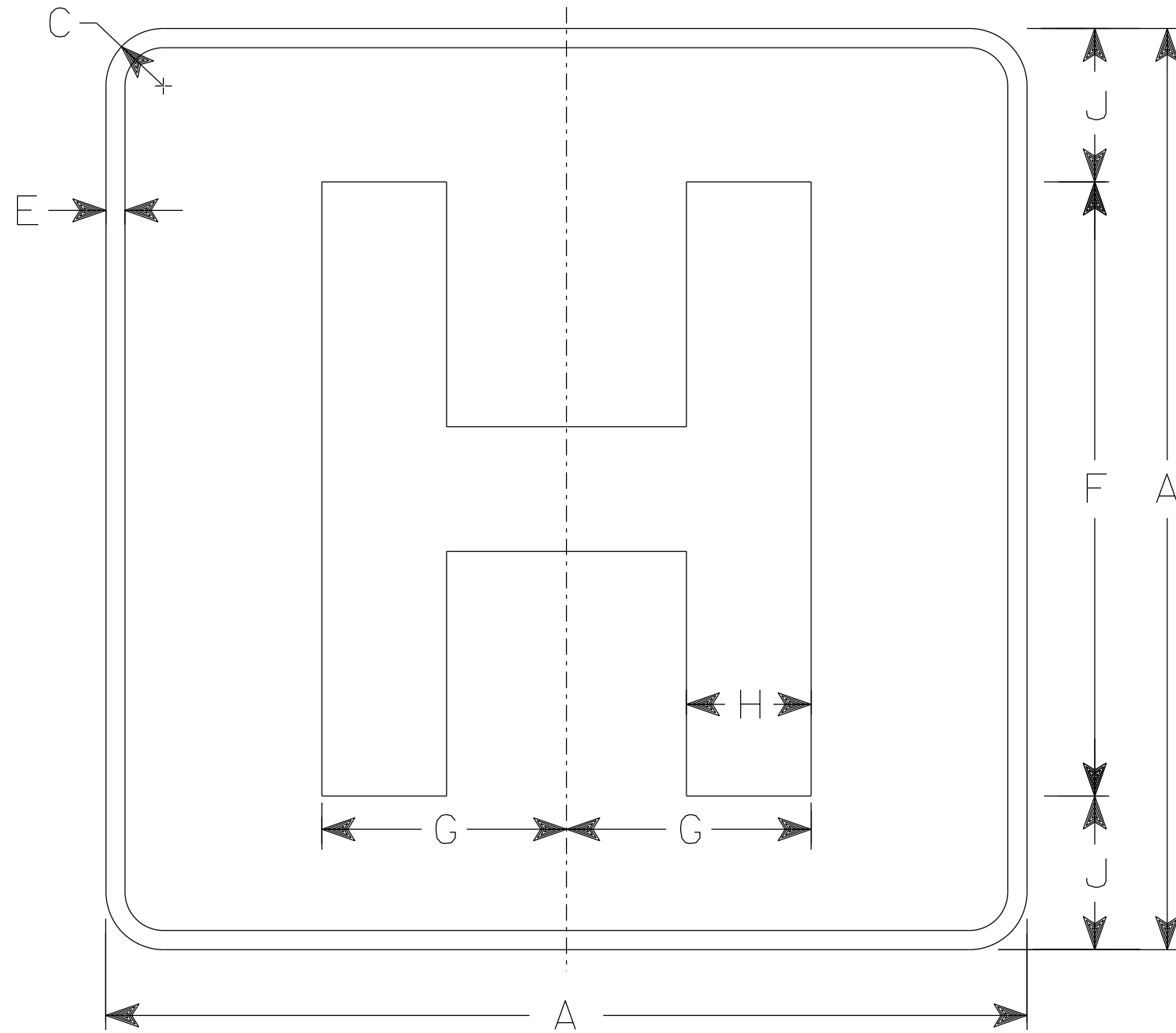
✱ LAG BOLTS SHALL BE 3/8" X 2 1/2"



BLOCK BANDING DETAIL (V-BLOCK OPTION)	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R Rauch</i> for State Traffic Engineer
DATE 4/19/2022	PLATE NO. A5-10.3

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - Blue
Message - White
3. Message Series - E Modified
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



D9-2

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18		1 1/2		1/2	12	4 3/4	2 3/8		3																	2.25
2S	24		1 1/2		1/2	16	6 3/8	3 1/4		4																	4.0
2M	24		1 1/2		1/2	16	6 3/8	3 1/4		4																	4.0
3	36		2 1/4		3/4	24	9 1/2	4 7/8		6																	9.0
4																											
5																											

STANDARD SIGN
D9-2

WISCONSIN DEPT OF TRANSPORTATION

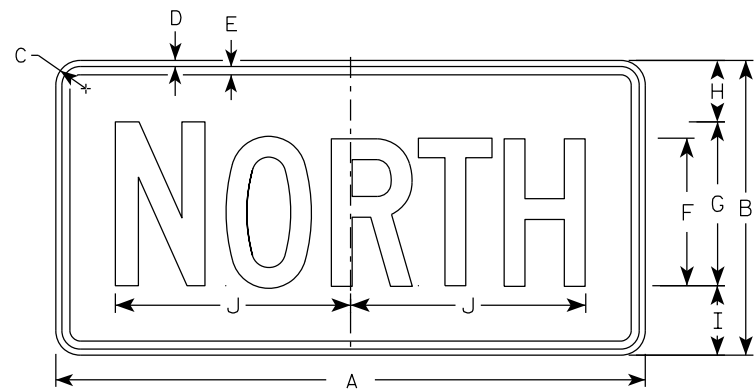
APPROVED *Matthew R Rauch*
For State Traffic Engineer

DATE 1/25/2023 PLATE NO. D9-2.6

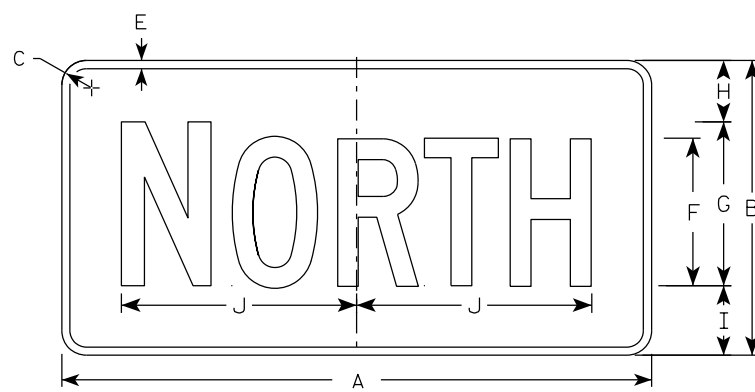
PROJECT NO:

SHEET NO:

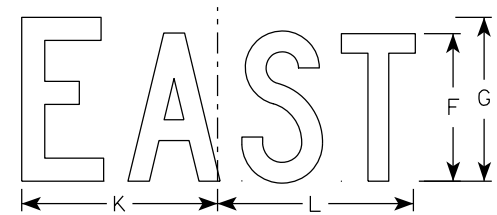
E



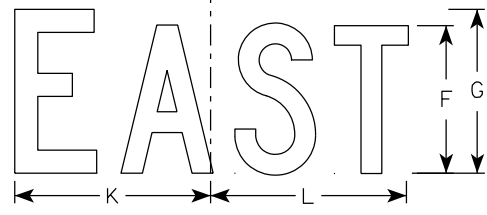
M3-1
MM3-1
MP3-1



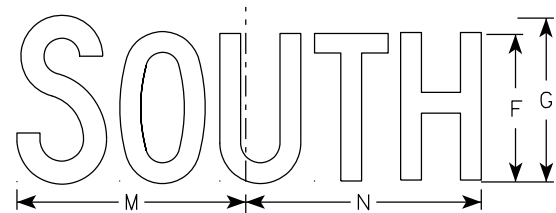
MB3-1
MK3-1
MN3-1



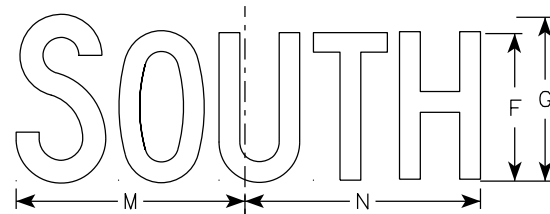
M3-2
MM3-2
MP3-2



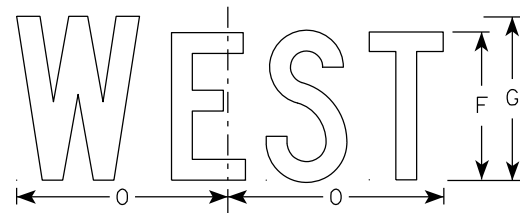
MB3-2
MK3-2
MN3-2



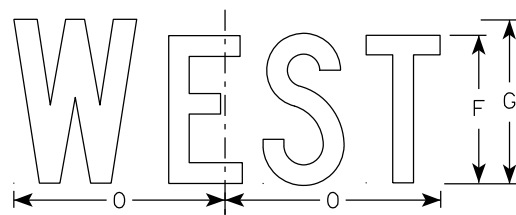
M3-3
MM3-3
MP3-3



MB3-3
MK3-3
MN3-3



M3-4
MM3-4
MP3-4



MB3-4
MK3-4
MN3-4

NOTES

- All Signs Type II - Type H Reflective
- Color:
 - Background - See note 5
 - Message - See note 5
- Message Series - C
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- M3-1 thru M3-4 Background - White
Message - Black
MB3-1 thru MB3-4 Background - Blue
Message - White
MK3-1 thru MK3-4 Background - Green
Message - White
MM3-1 thru MM3-4 Background - White
Message - Green
MN3-1 thru MN3-4 Background - Brown
Message - White
MP3-1 thru MP3-4 Background - White
Message - Blue
- Note the first letter of each direction is larger than the remainder of the message.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	12	1 1/2	3/8	3/8	6	7	2 1/4	2 3/4	10 1/4	7 7/8	8 3/8	10 1/4	9 3/4	8 3/4												2.00
2M	24	12	1 1/2	3/8	3/8	6	7	2 1/4	2 3/4	10 1/4	7 7/8	8 3/8	10 1/4	9 3/4	8 3/4												2.00
3	36	18	1 1/2	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13												4.5
4	36	18	1 1/2	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13												4.5
5	36	18	1 1/2	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13												4.5

STANDARD SIGNS
M3-1 THRU M3-4
SERIES

WISCONSIN DEPT OF TRANSPORTATION

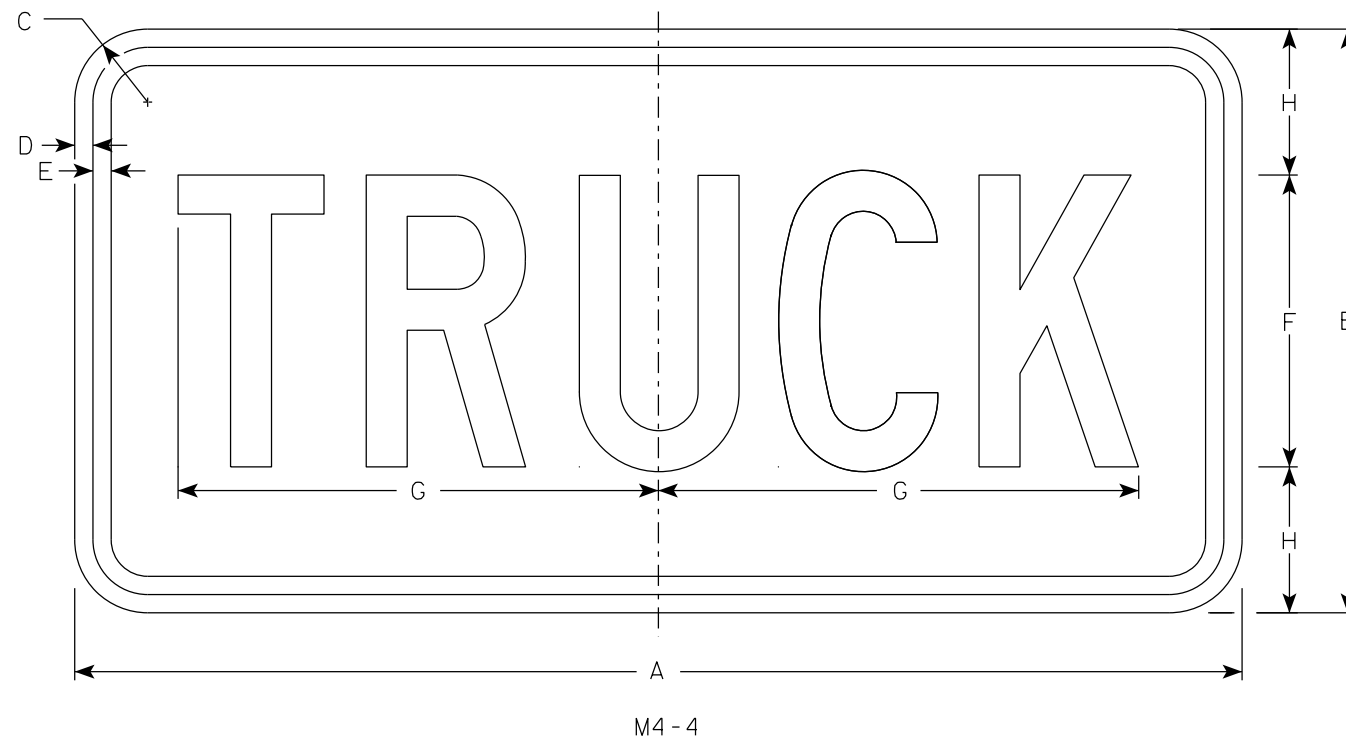
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 2/8/2023 PLATE NO. M3-1.15

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
 - Background - White
 - Message - Black
3. Message Series - C



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	12	1 1/2	3/8	3/8	6	9 7/8	3																			2.0
2M	24	12	1 1/2	3/8	3/8	6	9 7/8	3																			2.0
3	36	18	1 1/2	3/8	1/2	9	15 5/8	4 1/2																			4.5
4	36	18	1 1/2	3/8	1/2	9	15 5/8	4 1/2																			4.5
5	36	18	1 1/2	3/8	1/2	9	15 5/8	4 1/2																			4.5

STANDARD SIGN
M4-4

WISCONSIN DEPT OF TRANSPORTATION

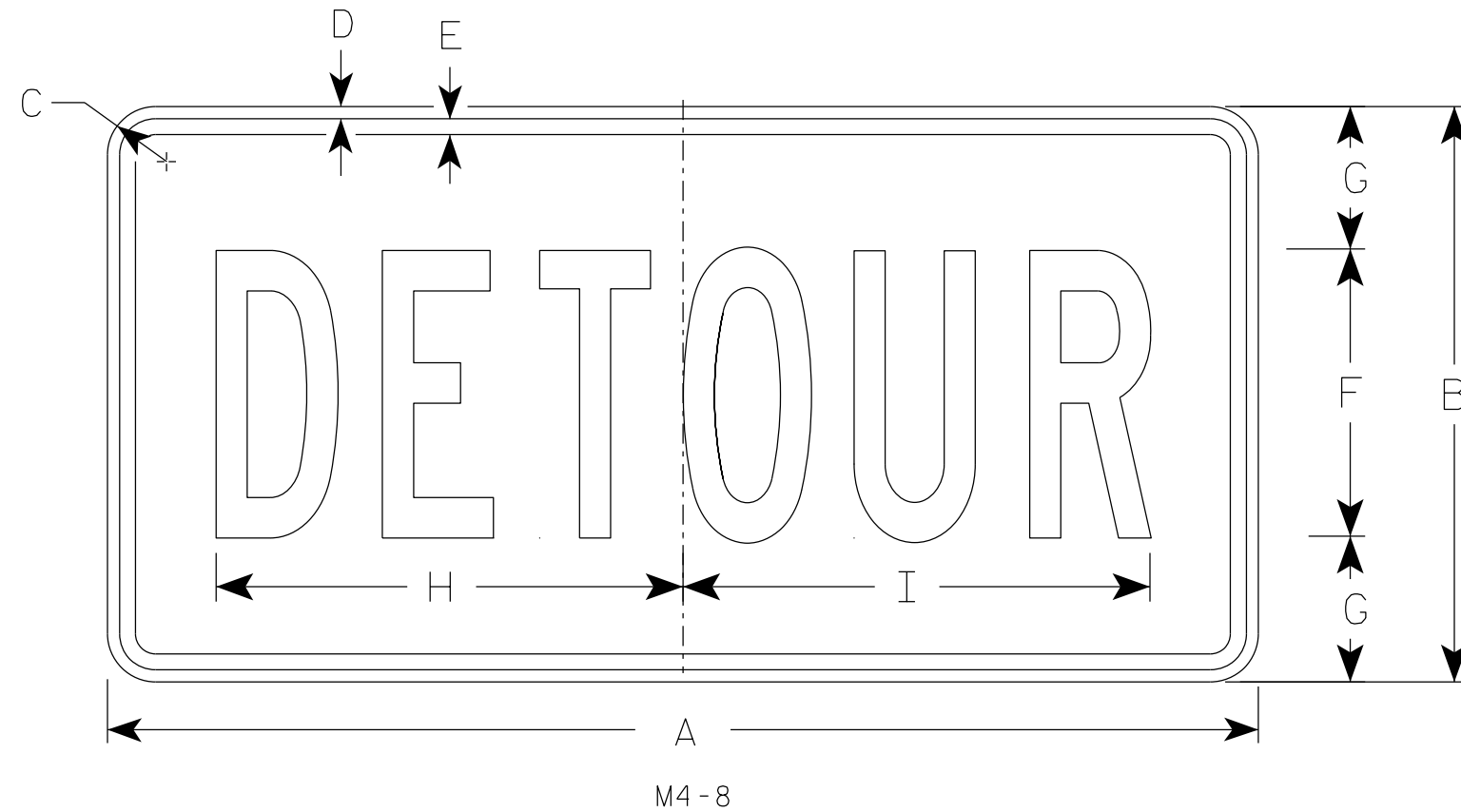
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 11/21/2022 PLATE NO. M4-4.4

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - B
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	12	1 1/2	3/8	3/8	6	3	10	10 1/4																		2.0
2M	24	12	1 1/2	3/8	3/8	6	3	10	10 1/4																		2.0
3	36	18	1 1/2	3/8	1/2	9	4 1/2	14 5/8	14 1/2																		4.5
4	36	18	1 1/2	3/8	1/2	9	4 1/2	14 5/8	14 1/2																		4.5
5	36	18	1 1/2	3/8	1/2	9	4 1/2	14 5/8	14 1/2																		4.5

STANDARD SIGN
M4-8

WISCONSIN DEPT OF TRANSPORTATION

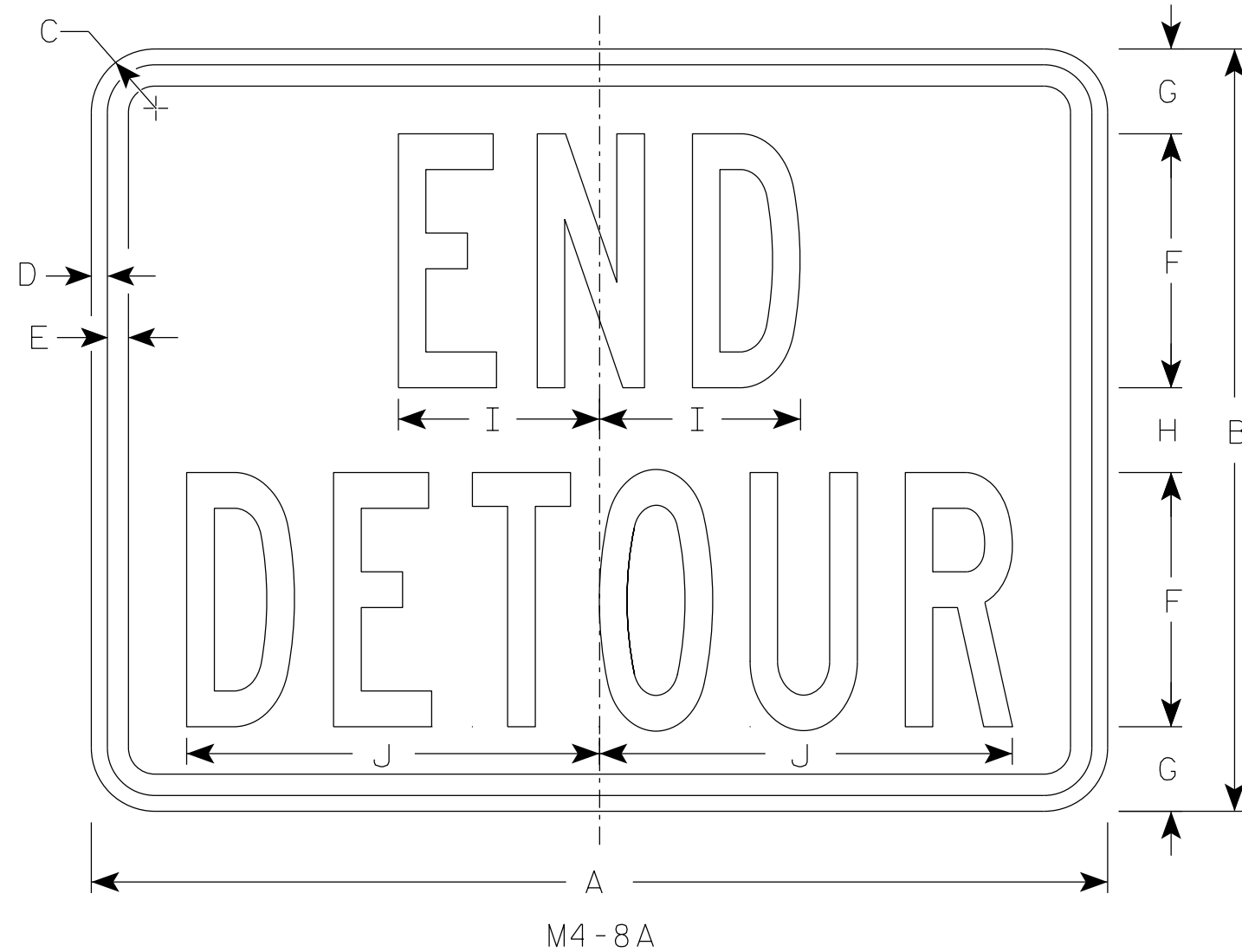
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 2/9/2023 PLATE NO. M4-8.4

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ **E**

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - B
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



M4-8A

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	18	1 1/2	3/8	1/2	6	2	2	4 3/4	9 3/4																	3.0
2M	24	18	1 1/2	3/8	1/2	6	2	2	4 3/4	9 3/4																	3.0
3	30	24	1 1/2	3/8	1/2	8	2 1/2	3	6 3/4	13																	5.0
4	30	24	1 1/2	3/8	1/2	8	2 1/2	3	6 3/4	13																	5.0
5	30	24	1 1/2	3/8	1/2	8	2 1/2	3	6 3/4	13																	5.0

STANDARD SIGN
M4-8A

WISCONSIN DEPT OF TRANSPORTATION

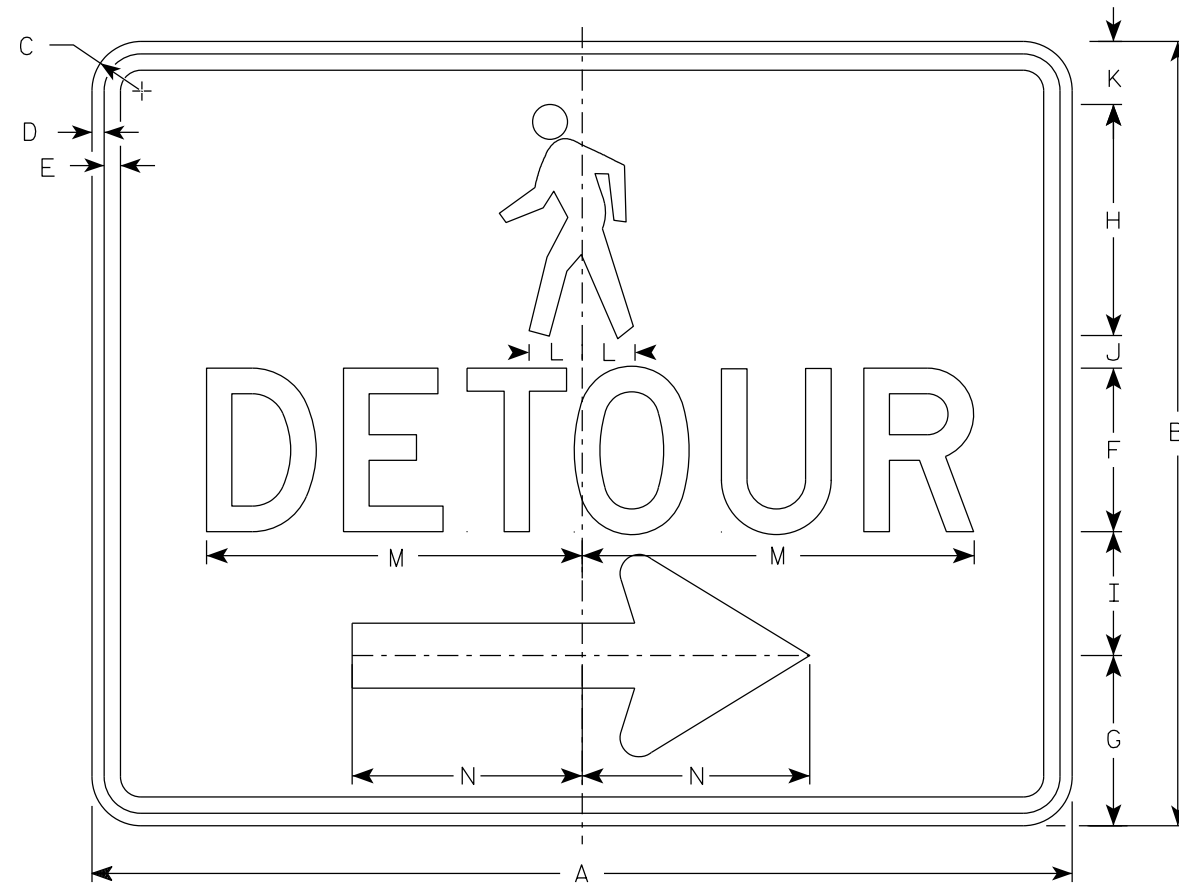
APPROVED *Matthew R Rauch*
For State Traffic Engineer

DATE 2/9/2023 PLATE NO. M4-8A.4

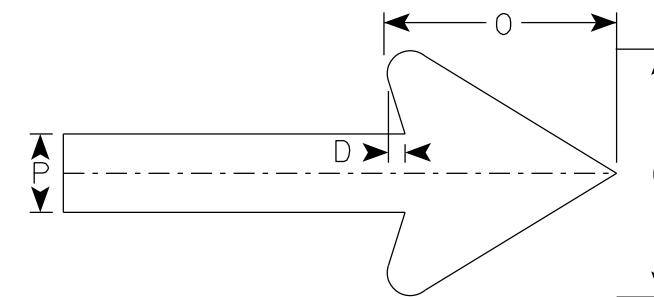
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ **E**

NOTES

1. Sign is Type II-Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. M4-9BL is the same as M4-9BR except the arrow is reversed.



M4-9BR



Arrow Detail

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30	24	1 1/2	3/8	1/2	5	5 1/4	7 1/8	3 3/4	1	1 1/8	1 5/8	11 3/4	7	6	2											5.0
2M	30	24	1 1/2	3/8	1/2	5	5 1/4	7 1/8	3 3/4	1	1 1/8	1 5/8	11 3/4	7	6	2											5.0
3																											
4																											
5																											

STANDARD SIGN
M4-9B L&R

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

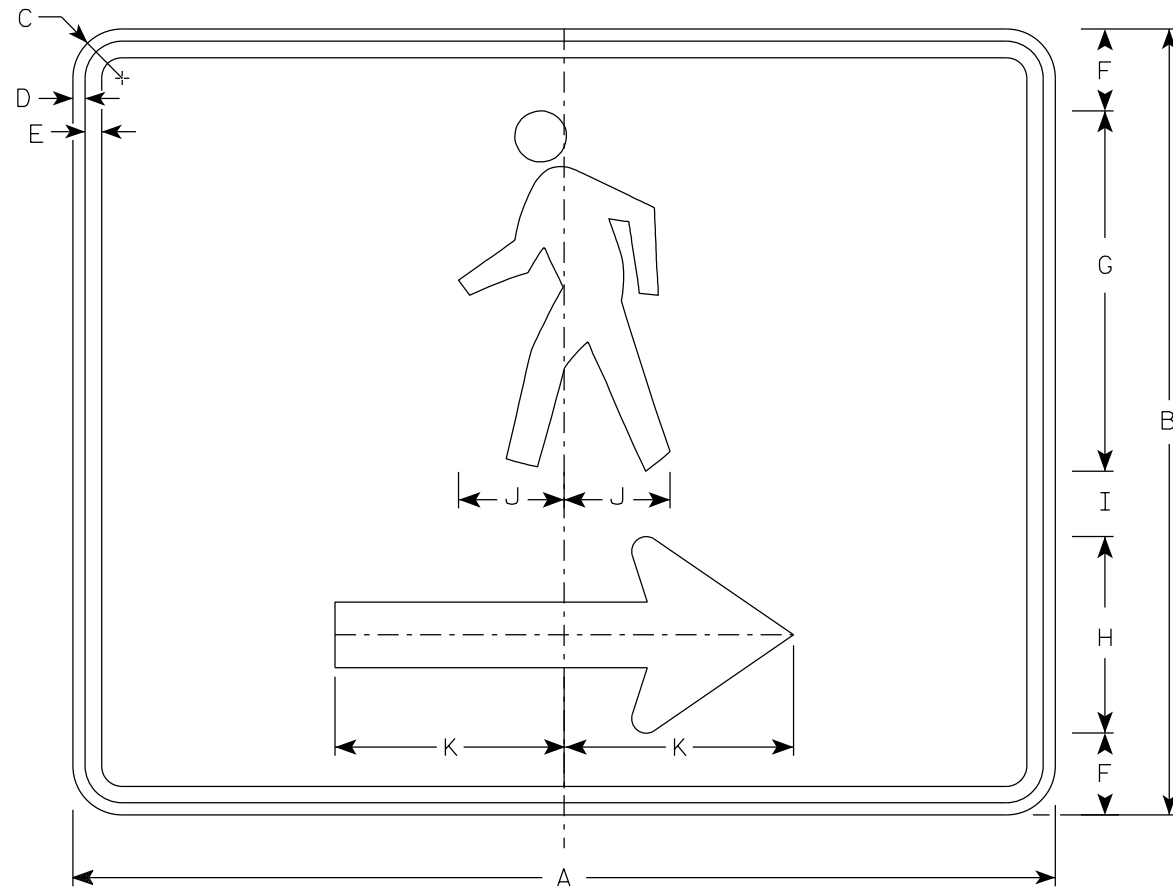
DATE 2/9/2023 PLATE NO. M4-9B.4

7

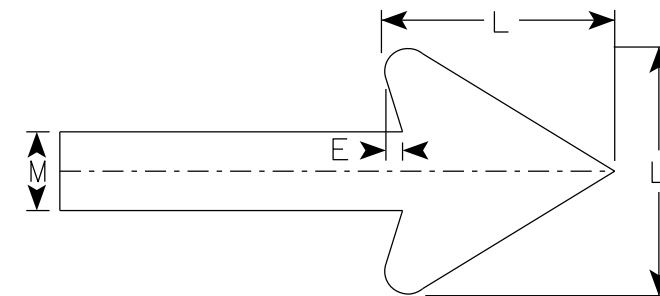
7

NOTES

1. Sign is Type II- Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. M4-60L is the same as M4-60R except the arrow is reversed.



M4-60R



Arrow Detail

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30	24	1 1/2	3/8	1/2	2 1/2	11	6	2	3 1/4	7	6	2														5.00
2M	30	24	1 1/2	3/8	1/2	2 1/2	11	6	2	3 1/4	7	6	2														5.00
3																											
4																											
5																											

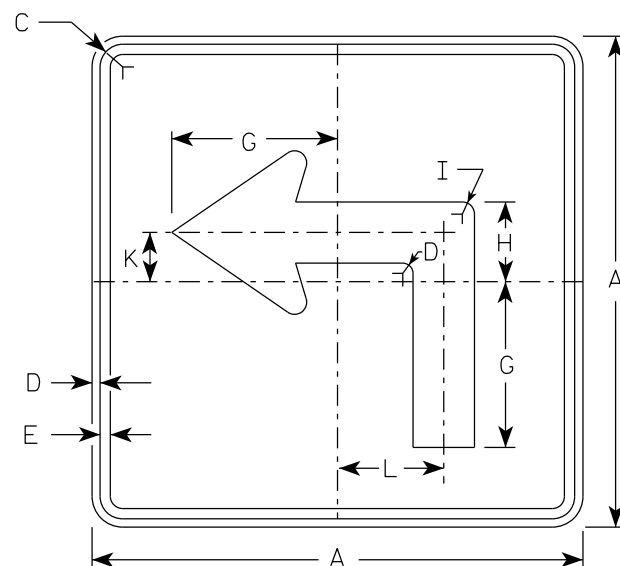
STANDARD SIGN
M4-60 L&R

WISCONSIN DEPT OF TRANSPORTATION

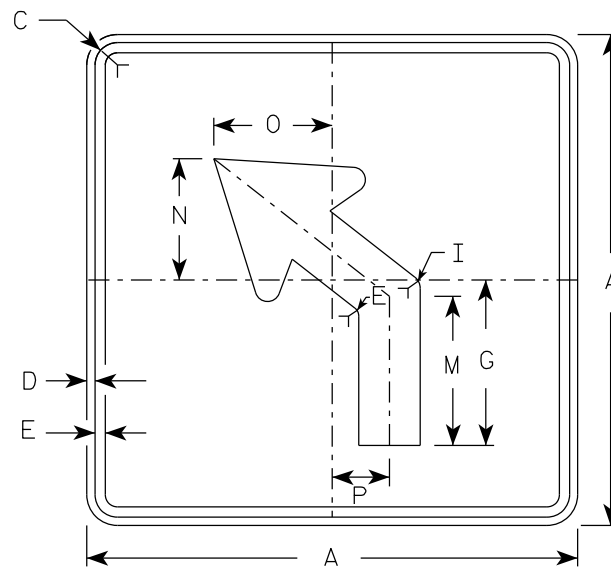
APPROVED *Matthew R Rauch*
For State Traffic Engineer

DATE 2/14/2023 PLATE NO. M4-60.2

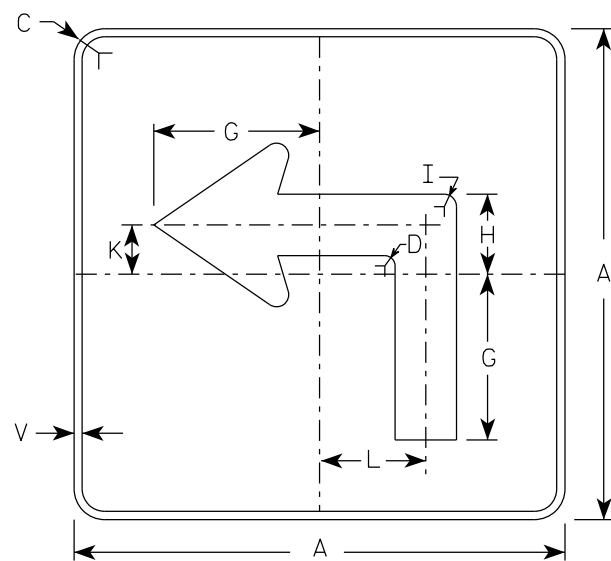
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ **E**



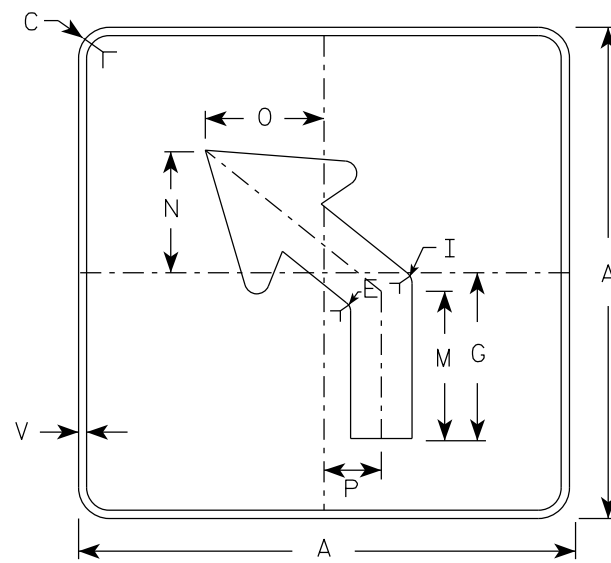
M5-1L
MM5-1L
M05-1L
MP5-1L



M5-2L
MM5-2L
M05-2L
MP5-2L

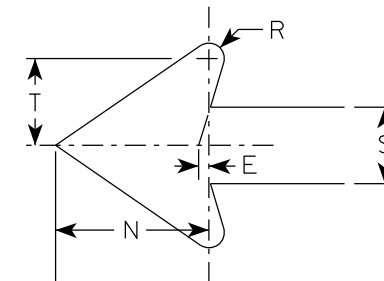


MB5-1L
MK5-1L
MN5-1L
MR5-1L



MB5-2L
MK5-2L
MN5-2L
MR5-2L

ARROW DETAIL



NOTES

- Signs are Type II - Type H reflective except as shown
- Color:
Background - See note 4
Message - See note 4
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- | | |
|-----------------|---|
| M5-1 and M5-2 | Background - White |
| | Message - Black |
| MB5-1 and MB5-2 | Background - Blue |
| | Message - White |
| MK5-1 and MK5-2 | Background - Green |
| | Message - White |
| MM5-1 and MM5-2 | Background - White |
| | Message - Green |
| MN5-1 and MN5-2 | Background - Brown |
| | Message - White |
| M05-1 and M05-2 | Background - Orange - Type F Reflective |
| | Message - Black |
| MP5-1 and MP5-2 | Background - White |
| | Message - Blue |
| MR5-1 and MR5-2 | Background - Brown |
| | Message - Yellow |
- M5-1R same as M5-1L except arrow points right.
- M5-2R same as M5-2L except arrow tilts right.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	21		1 1/2	3/8	3/8		7	3 3/8	5/8		2 1/8	4 1/2	6 3/8	5 1/4	5	2 1/2		1/2	2 5/8	3		1/2					3.06
2M	21		1 1/2	3/8	3/8		7	3 3/8	5/8		2 1/8	4 1/2	6 3/8	5 1/4	5	2 1/2		1/2	2 5/8	3		1/2					3.06
3	30		1 7/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4		1/2					6.25
4	30		1 7/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4		1/2					6.25
5	30		1 7/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4		1/2					6.25

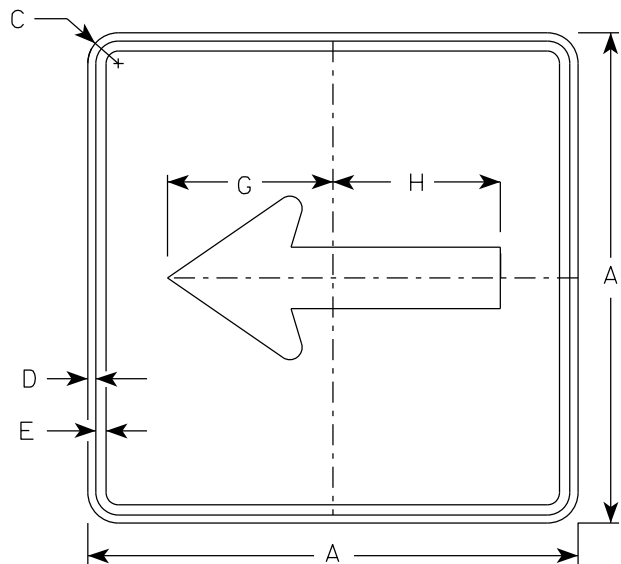
STANDARD SIGN
M5-1 & M5-2

WISCONSIN DEPT OF TRANSPORTATION

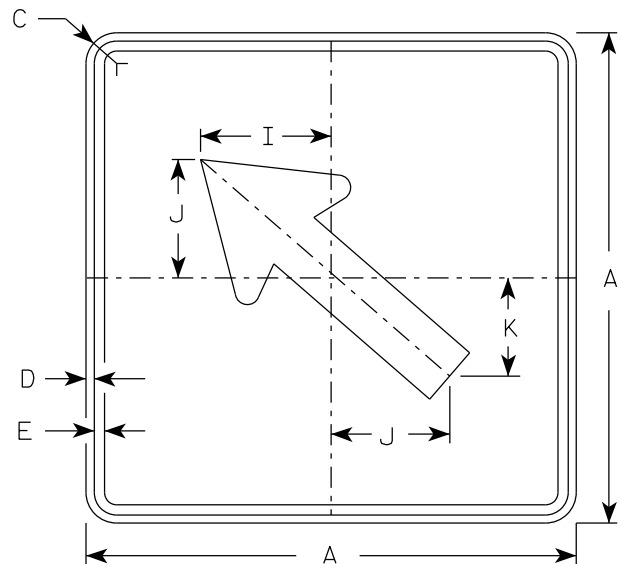
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 2/13/2023 PLATE NO. M5-1.15

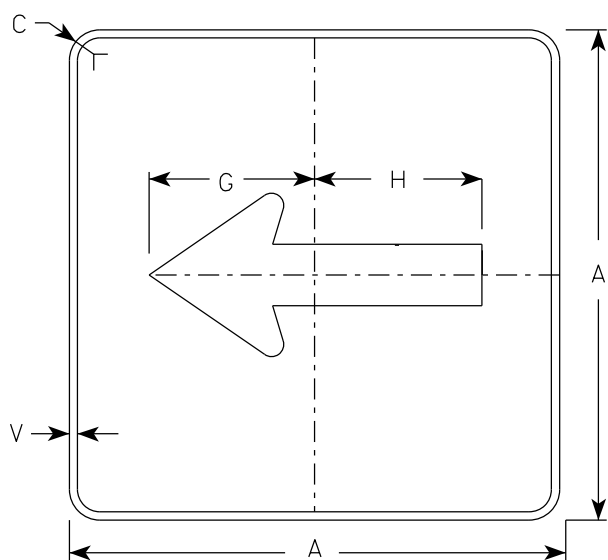
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**



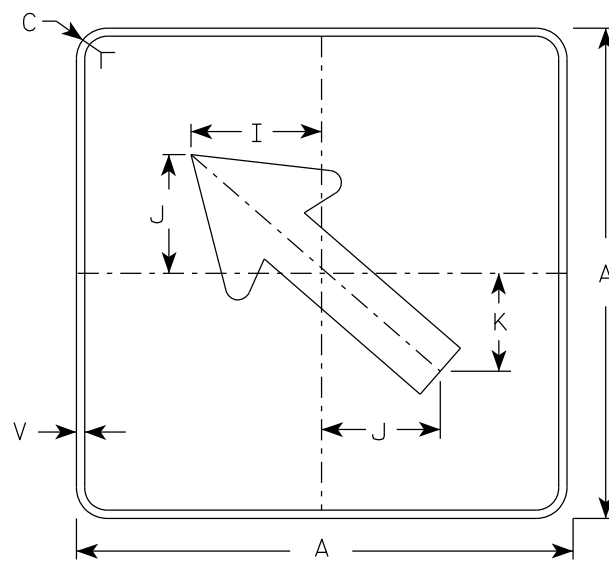
M6-1
MM6-1
M06-1
MP6-1



M6-2
MM6-2
M06-2
MP6-2



MB6-1
MK6-1
MN6-1
MR6-1

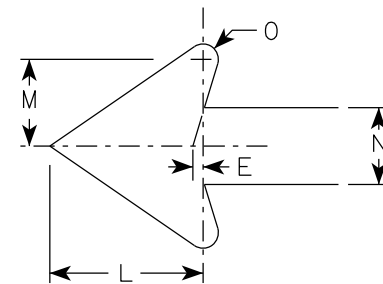


MB6-2
MK6-2
MN6-2
MR6-2

NOTES

- Signs are Type II - Type H Reflective except as Shown
- Color:
 - Background - See note 4
 - Message - See note 4
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- M6-1 and M6-2 Background - White
Message - Black
MB6-1 and MB6-2 Background - Blue
Message - White
MK6-1 and MK6-2 Background - Green
Message - White
MM6-1 and MM6-2 Background - White
Message - Green
MN6-1 and MN6-2 Background - Brown
Message - White
M06-1 and M06-2 Background - Orange - Type F Reflective
Message - Black
MP6-1 and MP6-2 Background - White
Message - Blue
MR6-1 and MR6-2 Background - Brown
Message - Yellow

ARROW DETAIL



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	21		1 1/2	3/8	3/8		7 1/2	7 1/8	5 5/8	5	4 1/4	5 1/4	3	2 5/8	1/2							1/2					3.06
2M	21		1 1/2	3/8	3/8		7 1/2	7 1/8	5 5/8	5	4 1/4	5 1/4	3	2 5/8	1/2							1/2					3.06
3	30		1 7/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4							1/2					6.25
4	30		1 7/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4							1/2					6.25
5	30		1 7/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4							1/2					6.25

STANDARD SIGN
M6-1 & M6-2
SERIES

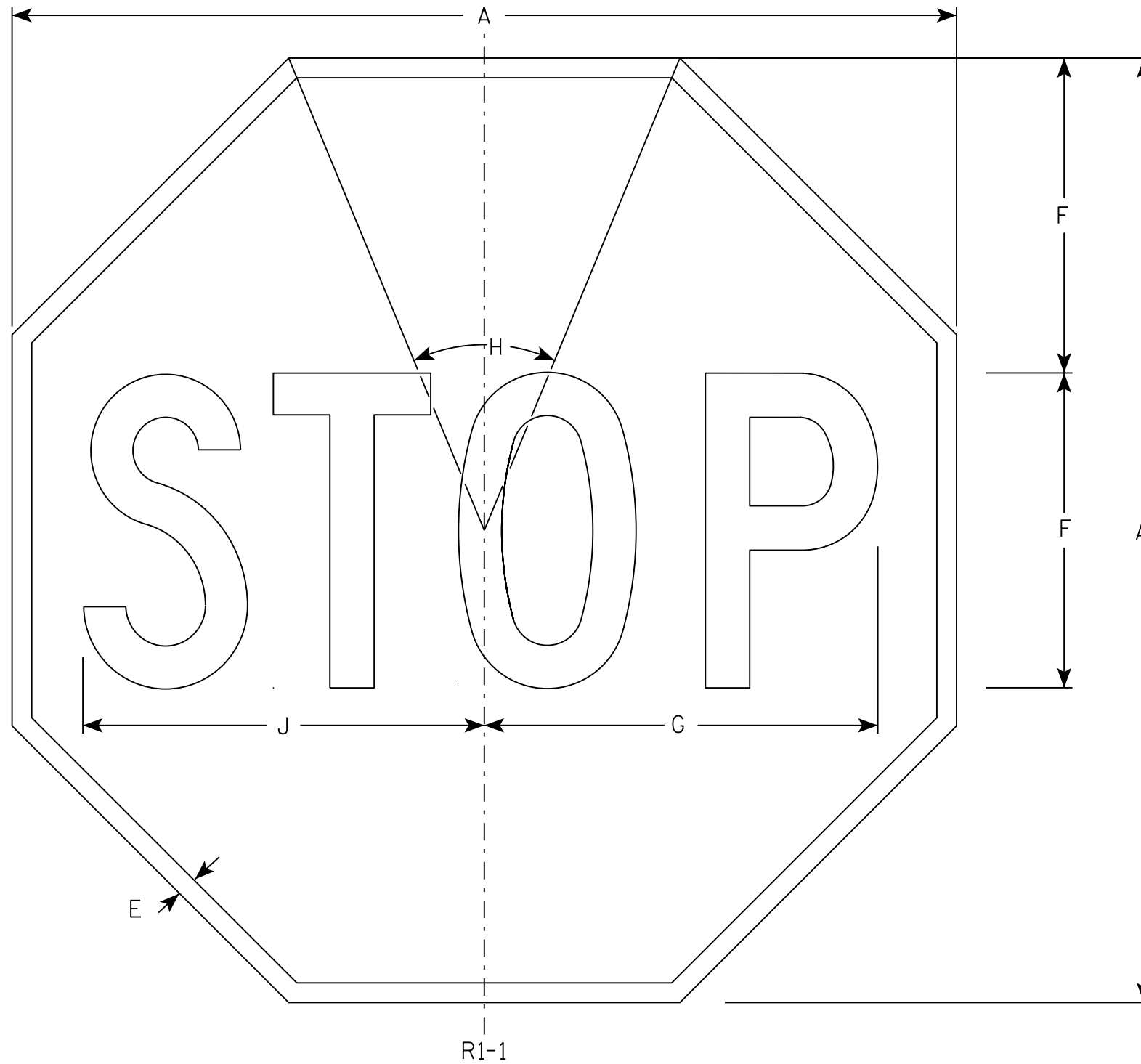
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Rauch*
For State Traffic Engineer

DATE 2/13/2023 PLATE NO. M6-1.16

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Red
Message - White
3. Message Series - C



R1-1

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30				5/8	10	12 1/2	45°		12 3/4																	5.18
2S	30				5/8	10	12 1/2	45°		12 3/4																	5.18
2M	36				3/4	12	15	45°		15 3/8																	7.46
3	36				3/4	12	15	45°		15 3/8																	7.46
4	48				1	16	20	45°		20 1/2																	13.25
5	48				1	16	20	45°		20 1/2																	13.25
6	18				3/8	6	7 3/4	45°		7 3/4																	1.86
7	12				1/4	4	5	45°		5 1/8																	0.78

STANDARD SIGN
R1-1

WISCONSIN DEPT OF TRANSPORTATION

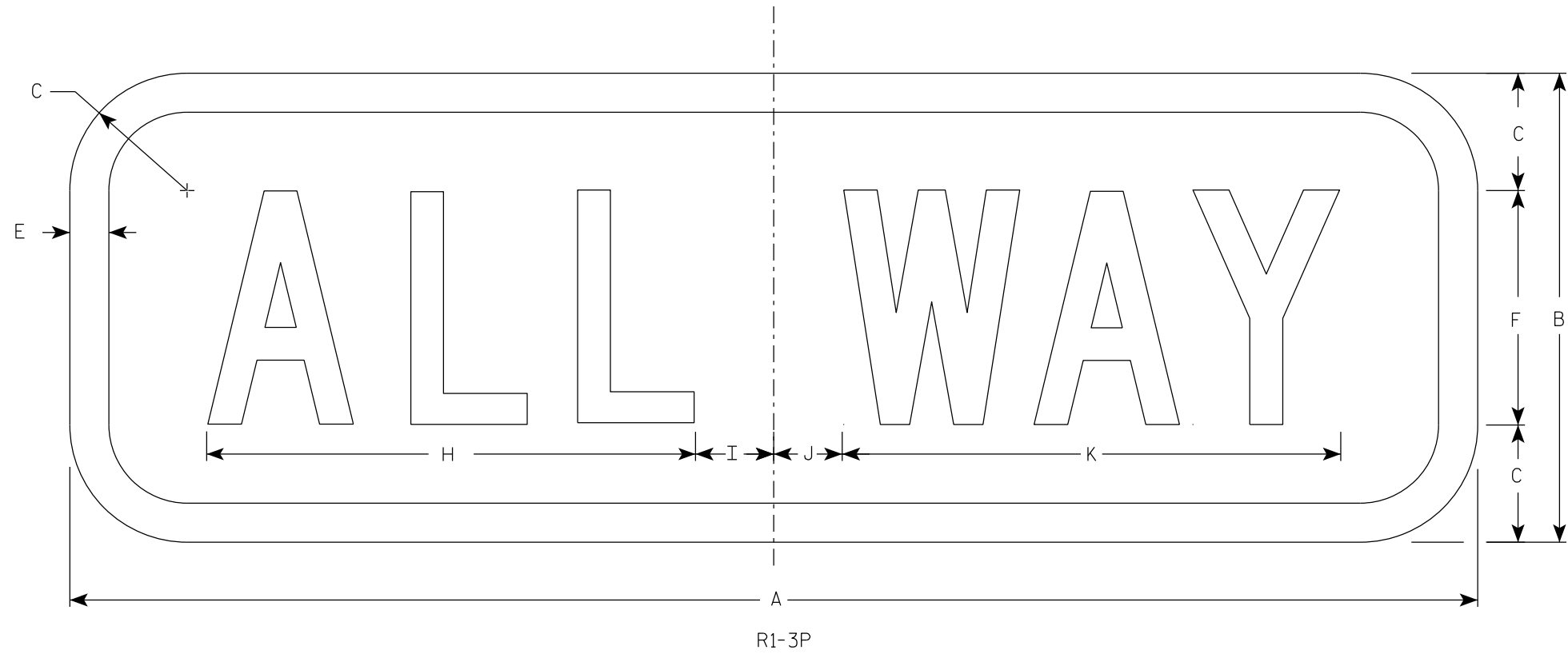
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 11/12/15 PLATE NO. R1-1.13

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - Red
Message - White
3. Message Series - C
4. For 30"x30" R1-1 use 18"x6" R1-3P sign
For 36"x36" R1-1 use 24"x9" R1-3P sign
For 48"x48" R1-1 use 30"x12" R1-3P sign



R1-3P

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18	6	1 1/2		1/2	3		6 1/4	1 1/4	7/8	6 3/8																0.75
2S	18	6	1 1/2		1/2	3		6 1/4	1 1/4	7/8	6 3/8																0.75
2M	24	9	1 1/2		1/2	5		9 1/4	1 1/4	3/4	9 3/4																1.5
3	24	9	1 1/2		1/2	5		9 1/4	1 1/4	3/4	9 3/4																1.5
4	30	12	1 1/2		5/8	6		11	2 1/4	1 1/2	11 3/4																2.5
5	30	12	1 1/2		5/8	6		11	2 1/4	1 1/2	11 3/4																2.5

STANDARD SIGN
R1-3P

WISCONSIN DEPT OF TRANSPORTATION

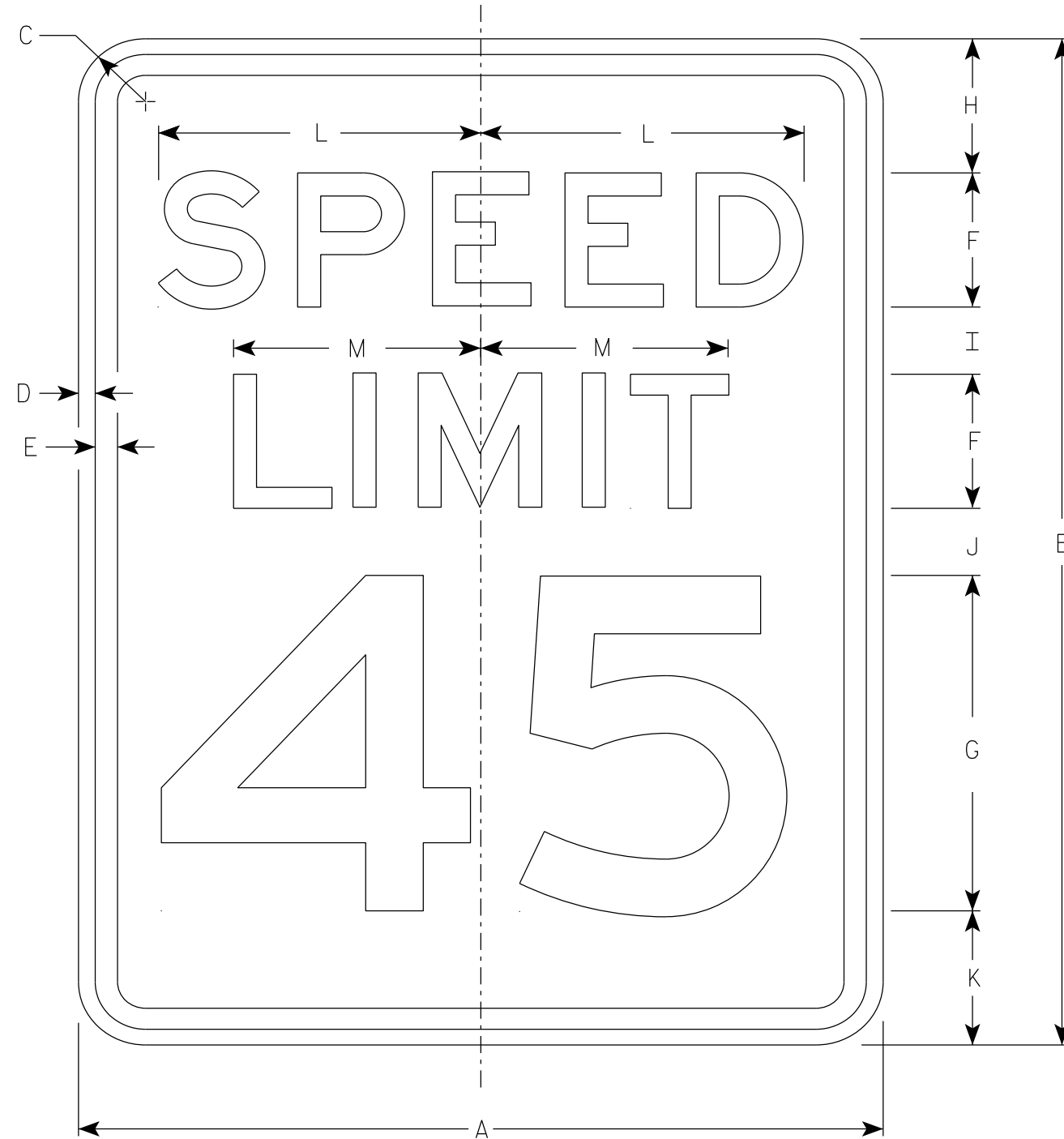
APPROVED *Matthew R Rauch*
for State Traffic Engineer

DATE 1/19/23 PLATE NO. R1-3P.4

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - E
4. Substitute appropriate numerals and optically adjust spacing to achieve proper balance.



R2-1

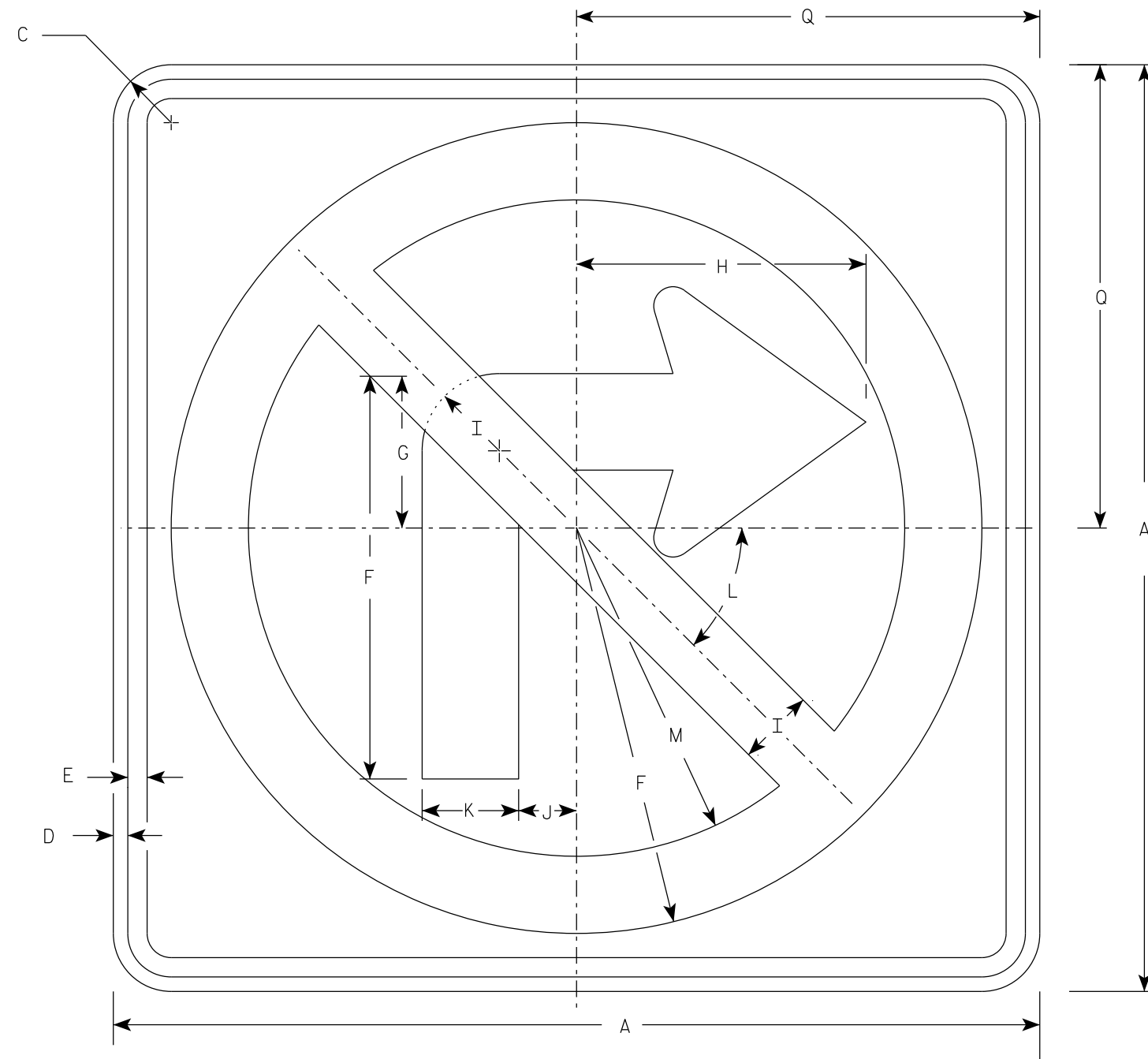
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18	24	1 1/2	3/8	1/2	3	8	3	2	2	3	7 1/4	5 1/2														3.0
2S	24	30	1 1/2	3/8	1/2	4	10	3	2 1/4	3 3/8	3 3/8	9 5/8	7 3/8														5.0
2M	30	36	1 7/8	1/2	5/8	5	12	5	2 1/2	2 1/2	4	12	9 1/4														7.5
3	36	48	1 7/8	1/2	5/8	6	14	6	5	5	6	14 3/8	11														12.0
4	36	48	1 7/8	1/2	5/8	6	14	6	5	5	6	14 3/8	11														12.0
5	48	60	3	3/4	1	8	20	6	4 1/2	6 3/4	6 3/4	19 1/4	14 5/8														20.0

STANDARD SIGN
R2-1

WISCONSIN DEPT OF TRANSPORTATION

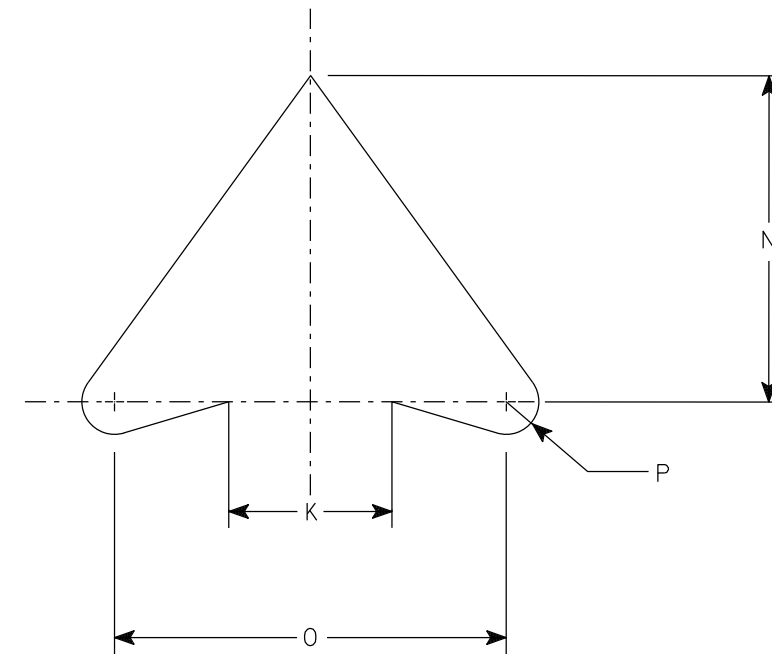
APPROVED *Matthew R Rauch*
State Traffic Engineer

DATE 2/1/23 PLATE NO. R2-1.14



NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - See note 3
3. Border & Arrow are non reflective black, the circle with diagonal bar is reflective red.



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/2	3/8	1/2	10 1/2	4	7 1/2	2	1 1/2	2 1/2	45	8 1/2	5	6	1/2	12										4.0
2S	24		1 1/2	3/8	1/2	10 1/2	4	7 1/2	2	1 1/2	2 1/2	45°	8 1/2	5	6	1/2	12										4.0
2M	36		2 1/4	5/8	3/4	15 3/4	6	11 1/4	3	2 1/4	3 3/4	45	12 3/4	7 1/2	9	3/4	18										9.0
3	36		2 1/4	5/8	3/4	15 3/4	6	11 1/4	3	2 1/4	3 3/4	45	12 3/4	7 1/2	9	3/4	18										9.0
4	36		2 1/4	5/8	3/4	15 3/4	6	11 1/4	3	2 1/4	3 3/4	45°	12 3/4	7 1/2	9	3/4	18										9.0
5	48		2 1/4	3/4	1	21	8	15	4	3	5	45°	17	10	12	1	24										16.0

STANDARD SIGN
R3-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

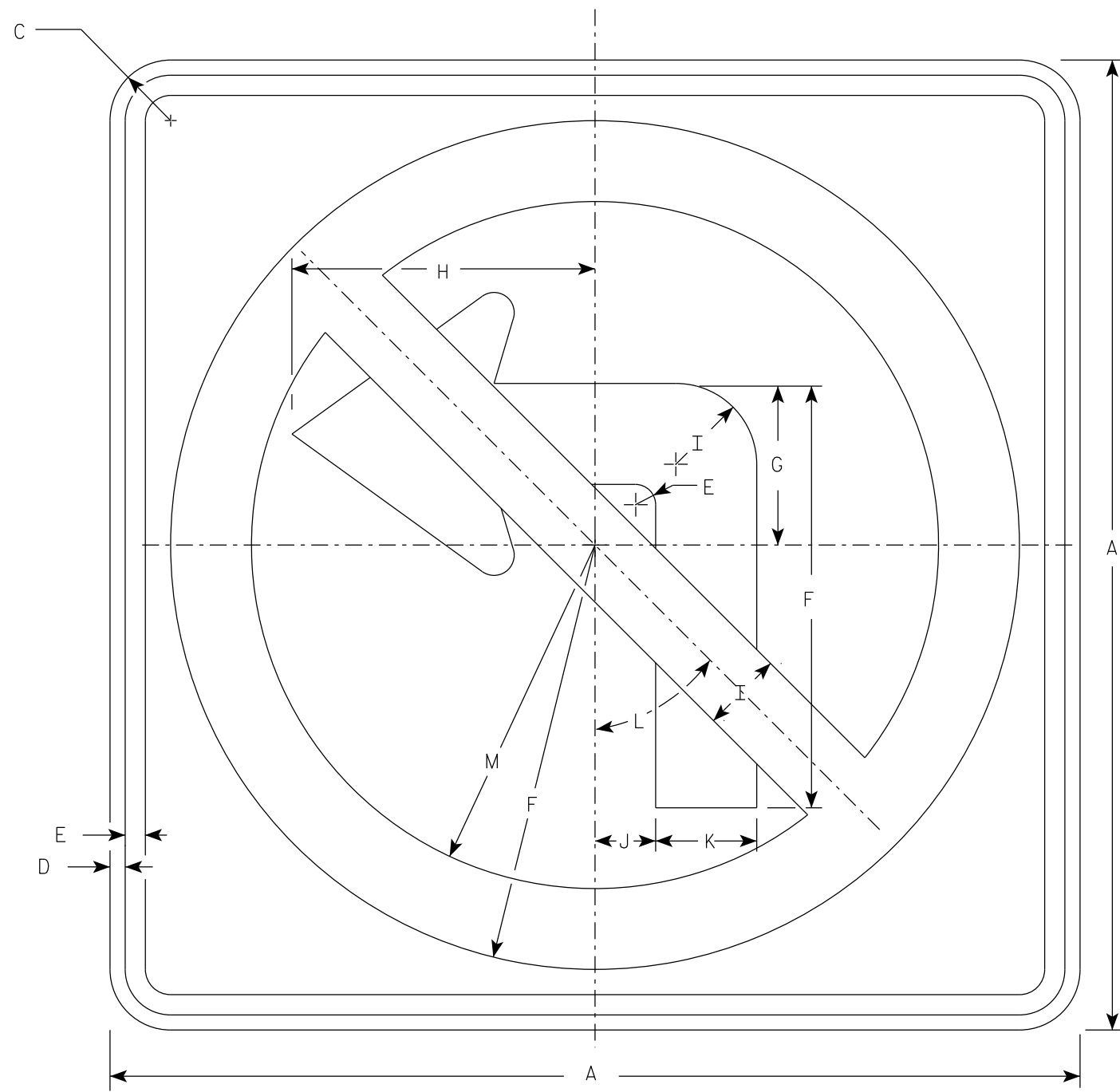
DATE 2/2/23 PLATE NO. R3-1.6

7

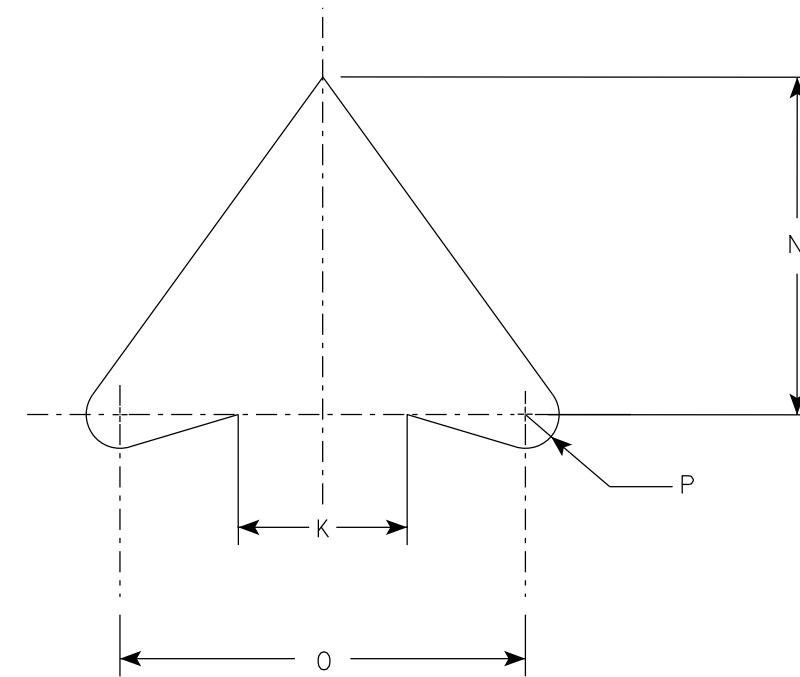
7

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - See note 3
3. Border & Arrow are non reflective black, the circle with diagonal bar is reflective red.



R3-2



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/2	3/8	1/2	10 1/2	4	7 1/2	2	1 1/2	2 1/2	45°	8 1/2	5	6	1/2											4.0
2S	24		1 1/2	3/8	1/2	10 1/2	4	7 1/2	2	1 1/2	2 1/2	45°	8 1/2	5	6	1/2											4.0
2M	36		2 1/4	5/8	3/4	15 3/4	6	11 1/4	3	2 1/4	3 3/4	45°	12 3/4	7 1/2	9	3/4											9.0
3	36		2 1/4	5/8	3/4	15 3/4	6	11 1/4	3	2 1/4	3 3/4	45°	12 3/4	7 1/2	9	3/4											9.0
4	36		2 1/4	5/8	3/4	15 3/4	6	11 1/4	3	2 1/4	3 3/4	45°	12 3/4	7 1/2	9	3/4											9.0
5	48		2 1/4	3/4	1	21	8	15	4	3	5	45°	17	10	12	1											16.0

STANDARD SIGN
R3-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 2/2/23 PLATE NO. R3-2.11

PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E

NOTES

1. Sigs are Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - D
4. Use appropriate Letter for Sign Code
Each letter added makes sign wider. Example R3-8EAR
5. Square footage of sign varies by letters

- | | |
|-------------|----------------------------|
| 1 Letter = | 3.75 sq ft for Size 2 |
| | 6.0 sq ft for Size 3 |
| | 10.0 sq ft for Size 4 or 5 |
| 2 Letters = | 7.5 sq ft for Size 2 |
| | 12.0 sq ft for Size 3 |
| | 20.0 sq ft for Size 4 or 5 |
| 3 Letters = | 11.25 sq ft for Size 2 |
| | 18.0 sq ft for Size 3 |
| | 30.0 sq ft for Size 4 or 5 |
| 4 Letters = | 15.0 sq ft for Size 2 |
| | 24.0 sq ft for Size 3 |
| | 40.0 sq ft for Size 4 or 5 |
| 5 Letters = | 18.75 sq ft for Size 2 |
| | 30.0 sq ft for Size 3 |
| | 50.0 sq ft for Size 4 or 5 |
| 6 Letters = | 22.5 sq ft for Size 2 |
| | 36.0 sq ft for Size 3 |
| | 60.0 sq ft for Size 4 or 5 |

6. When letters C,D,G,J are used on the Left or Right end of the sign the Sq. Ft. changes.

Add the amounts when these letters are used:

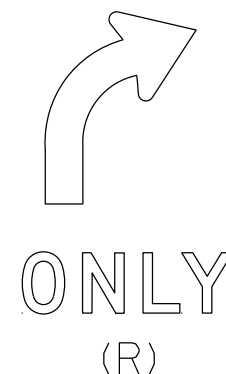
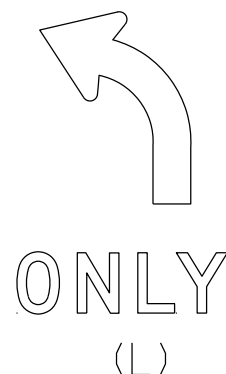
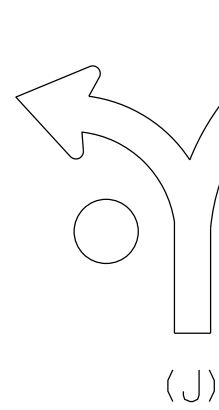
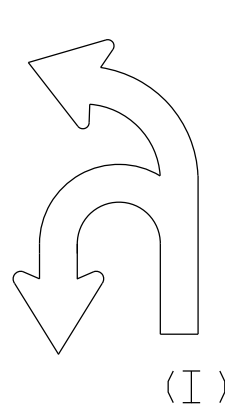
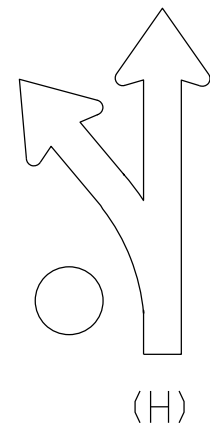
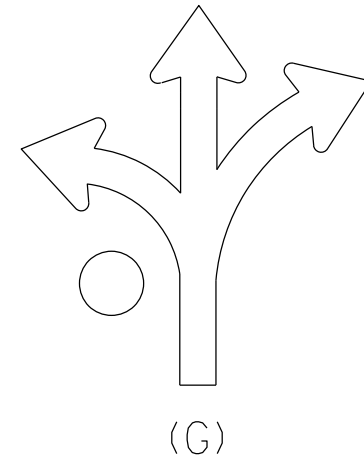
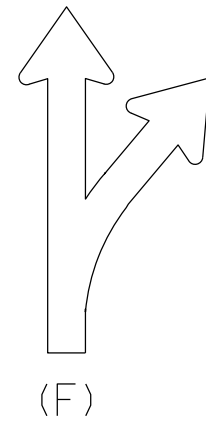
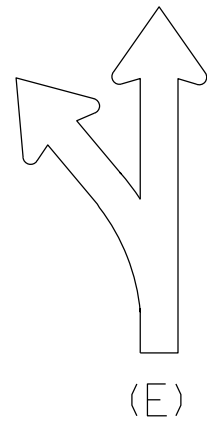
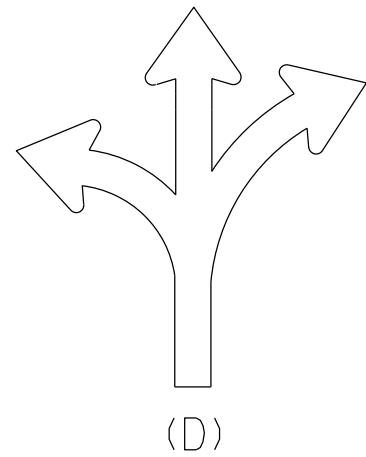
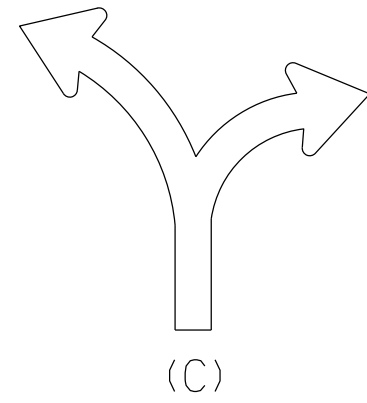
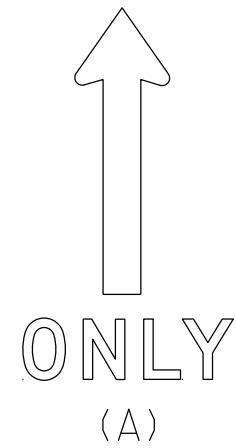
- 1.25 sq ft for Size 2
- 1.5 sq ft for Size 3
- 2.0 sq ft for Size 4 or 5

STANDARD SIGN
R3-8 Series

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Raub*
for State Traffic Engineer

DATE 2/14/23 PLATE NO. R3-8.2

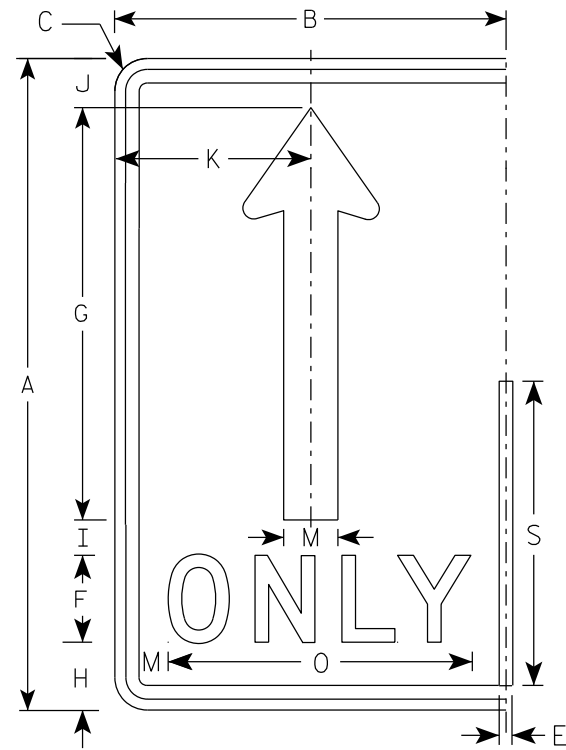


7

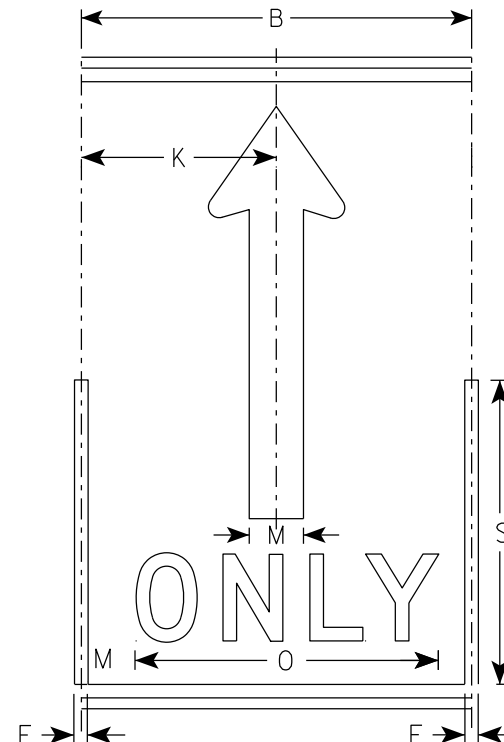
7

NOTES

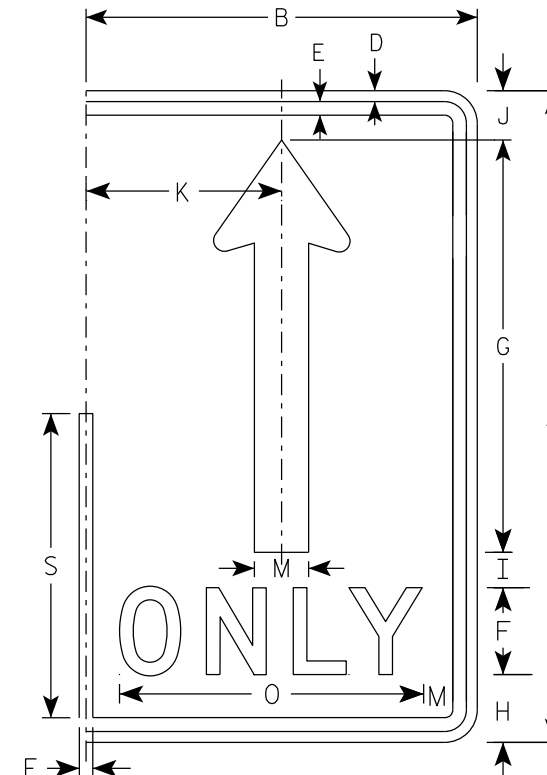
1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - D



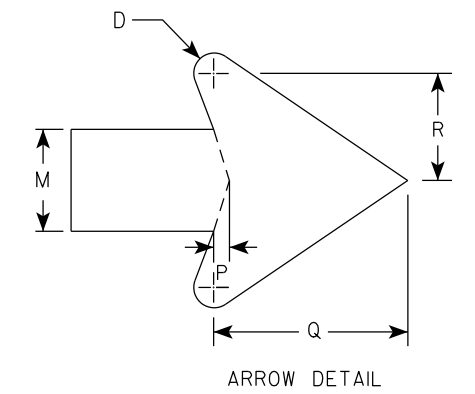
(A)



(A)



(A)



ARROW DETAIL

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30	18	1 1/2	1/2	5/8	4	19	3 1/8	1 5/8	2 1/4	9		2 1/2		14	3/8	4 3/4	2 5/8	14								3.75
2M	30	18	1 1/2	1/2	5/8	4	19	3 1/8	1 5/8	2 1/4	9		2 1/2		14	3/8	4 3/4	2 5/8	14								3.75
3	36	24	1 1/2	1/2	5/8	5	22 3/4	3 3/4	1 3/4	2 3/4	12		3		17 5/8	1/2	5 3/4	3 1/8	16 3/4								6.0
4	48	30	2 1/4	3/4	1	6	30 3/8	5 1/8	2 7/8	3 5/8	15		4		21 3/4	5/8	7 5/8	4 1/4	22 3/8								10.0
5	48	30	2 1/4	3/4	1	6	30 3/8	5 1/8	2 7/8	3 5/8	15		4		21 3/4	5/8	7 5/8	4 1/4	22 3/8								10.0

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

STANDARD SIGN
R3-8 (A) Arrow

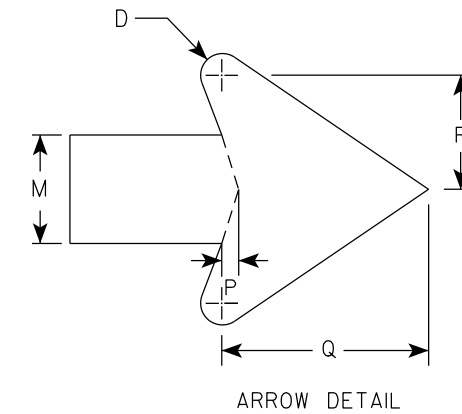
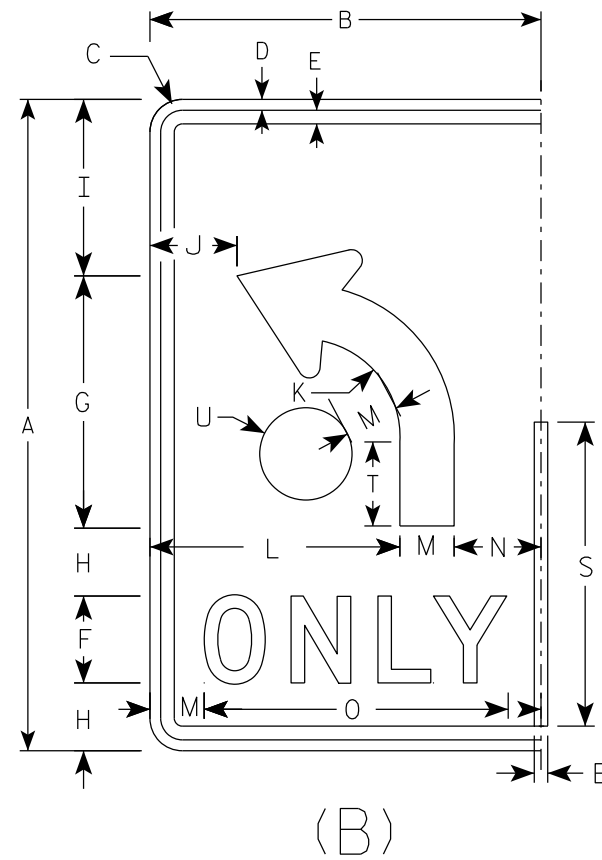
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Rauch*
for State Traffic Engineer

DATE 2/14/23 PLATE NO. R3-8.2

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
 - Background - White
 - Message - Black
 - Message Series - D



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30	18	1 1/2	1/2	5/8	4	11 5/8	3 1/8	8 1/8	4	4 1/2	11 1/2	2 1/2	4	14	3/8	4 3/4	2 5/8	14	3 7/8	2 1/8						3.75
2M	30	18	1 1/2	1/2	5/8	4	11 5/8	3 1/8	8 1/8	4	4 1/2	11 1/2	2 1/2	4	14	3/8	4 3/4	2 5/8	14	3 7/8	2 1/8						3.75
3	36	24	1 1/2	1/2	5/8	5	14	3 1/2	9 3/4	6	5 3/8	15	3	6	17 5/8	1/2	5 3/4	3 1/8	16 3/4	4 5/8	2 1/2						6.0
4	48	30	2 1/4	3/4	1	6	18 5/8	5 1/8	13 1/8	6 1/8	7 1/4	18	4	8	21 3/4	5/8	7 5/8	4 1/4	22 3/8	6 1/4	3 3/8						10.0
5	48	30	2 1/4	3/4	1	6	18 5/8	5 1/8	13 1/8	6 1/8	7 1/4	18	4	8	21 3/4	5/8	7 5/8	4 1/4	22 3/8	6 1/4	3 3/8						10.0

STANDARD SIGN
R3-8 (B) Arrow

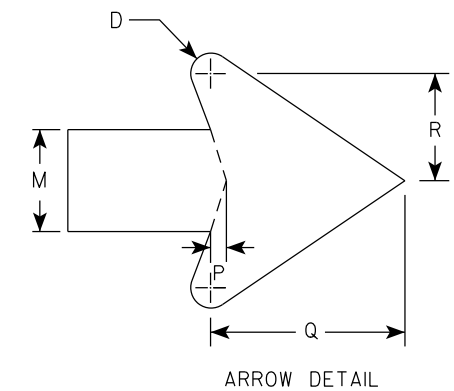
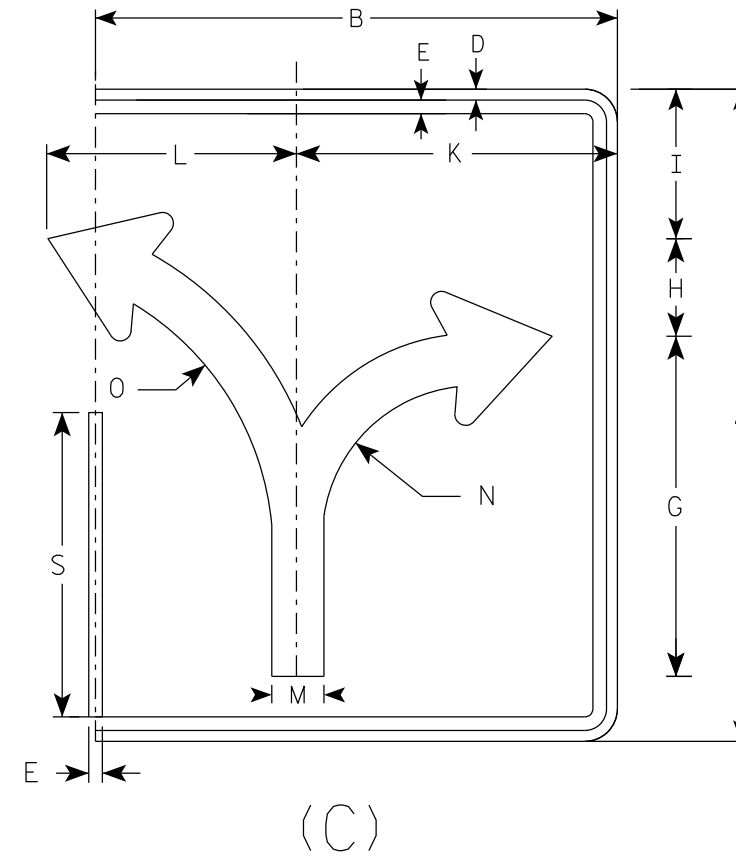
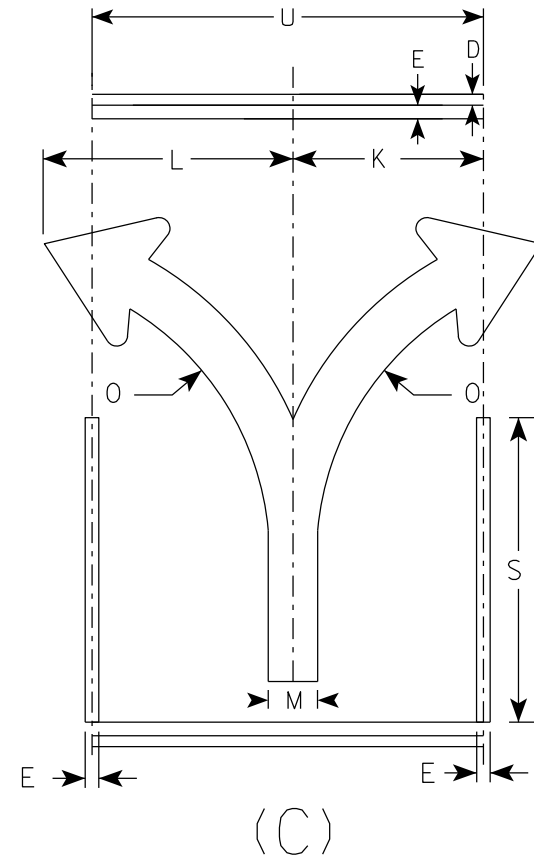
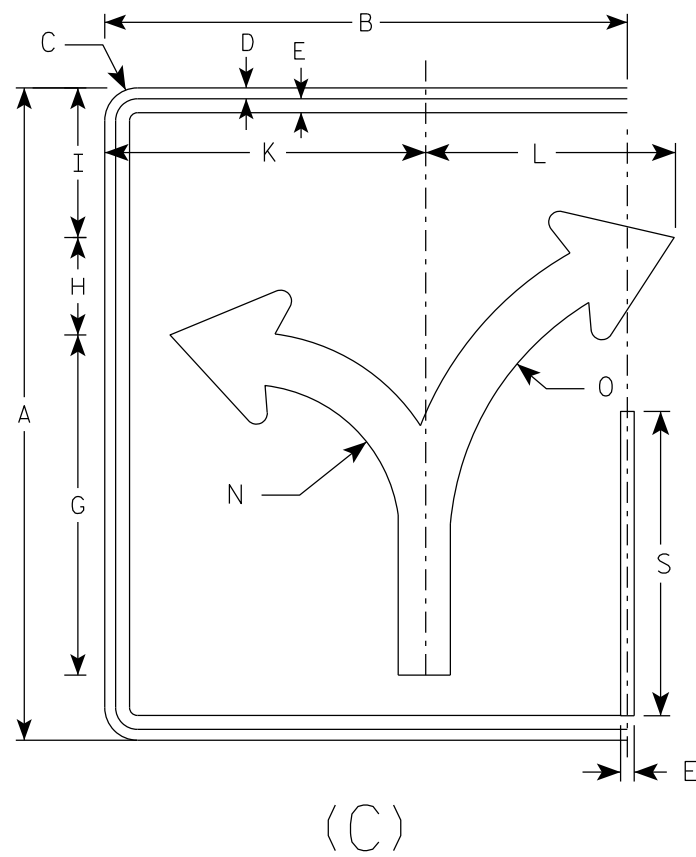
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 2/14/23 PLATE NO. R3-8.2

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - None



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	ENDS	MIDDLE
																											Area sq. ft.	Area sq. ft.
1																												
2S	30	24	1 1/2	1/2	5/8		15 5/8	4 1/2	6 7/8		14 3/4	11 1/2	2 3/8	7	13 1/4	3/8	4 1/2	2 1/2	14		18						5.0	3.75
2M	30	24	1 1/2	1/2	5/8		15 5/8	4 1/2	6 7/8		14 3/4	11 1/2	2 3/8	7	13 1/4	3/8	4 1/2	2 1/2	14		18					5.0	3.75	
3	36	30	1 1/2	1/2	5/8		18 3/4	5 1/2	8 1/4		17 1/4	17 1/4	2 7/8	8 3/8	16	1/2	5 1/2	3	16 3/4		24					7.5	6.0	
4	48	36	2 1/4	3/4	1		24 7/8	7 1/4	11		23 1/8	18	3 3/4	11 1/8	21 1/4	5/8	7 1/8	4	22 3/8		30					12.0	10.0	
5	48	36	2 1/4	3/4	1		24 7/8	7 1/4	11		23 1/8	18	3 3/4	11 1/8	21 1/4	5/8	7 1/8	4	22 3/8		30					12.0	10.0	

STANDARD SIGN
R3-8 (C) Arrow

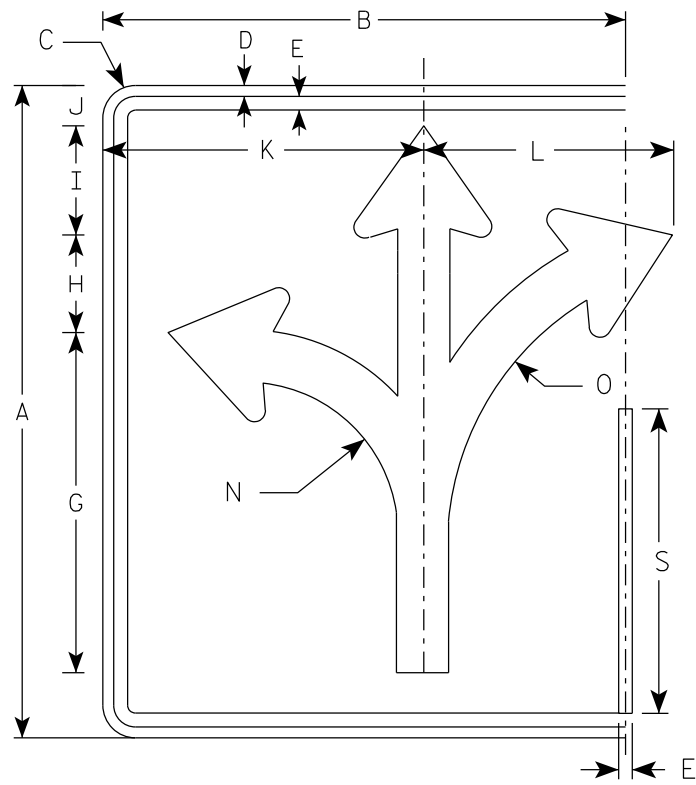
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

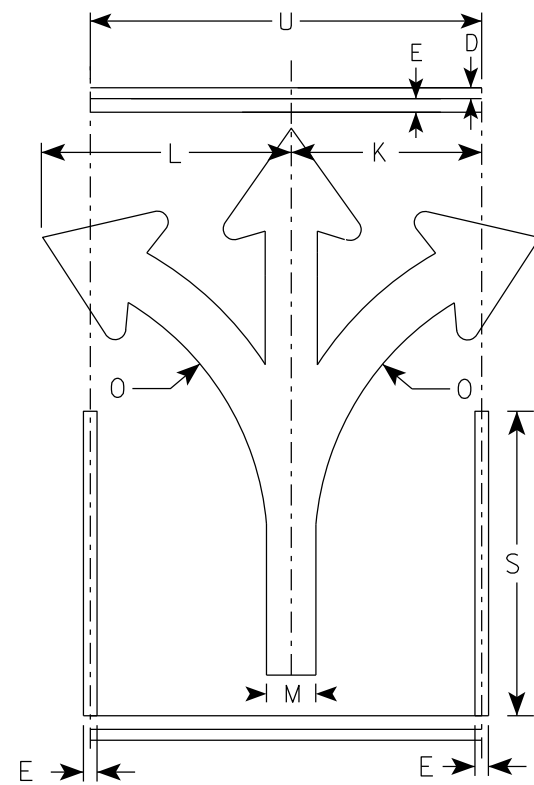
DATE 2/14/23 PLATE NO. R3-8.2

NOTES

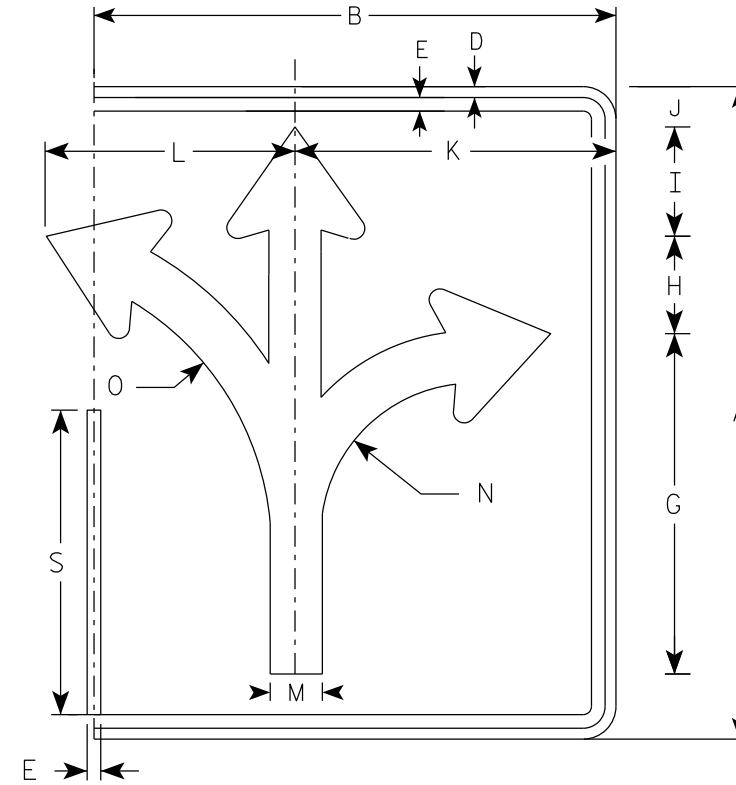
1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - None



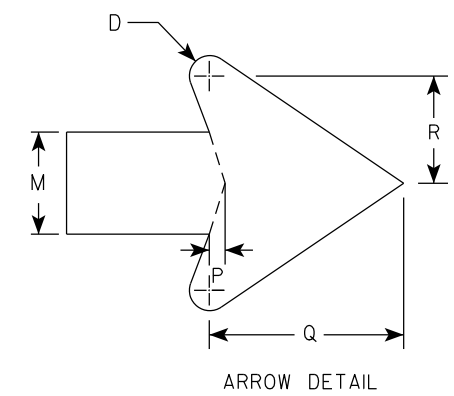
(D)



(D)



(D)



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	ENDS Area sq. ft.	MIDDLE Area sq. ft.
1																												
2S	30	24	1 1/2	1/2	5/8		15 5/8	4 1/2	5	1 7/8	14 3/4	11 1/2	2 3/8	7	13 1/4	3/8	4 1/2	2 1/2	14		18						5.0	3.75
2M	30	24	1 1/2	1/2	5/8		15 5/8	4 1/2	5	1 7/8	14 3/4	11 1/2	2 3/8	7	13 1/4	3/8	4 1/2	2 1/2	14		18						5.0	3.75
3	36	30	1 1/2	1/2	5/8		18 3/4	5 1/2	6	2 1/4	17 1/4	17 1/4	2 7/8	8 3/8	16	1/2	5 1/2	3	16 3/4		24						7.5	6.0
4	48	36	2 1/4	3/4	1		24 7/8	7 1/4	7 7/8	3 1/8	23 1/8	18	3 3/4	11 1/8	21 1/4	5/8	7 1/8	4	22 3/8		30						12.0	10.0
5	48	36	2 1/4	3/4	1		24 7/8	7 1/4	7 7/8	3 1/8	23 1/8	18	3 3/4	11 1/8	21 1/4	5/8	7 1/8	4	22 3/8		30						12.0	10.0

STANDARD SIGN
R3-8 (D) Arrow

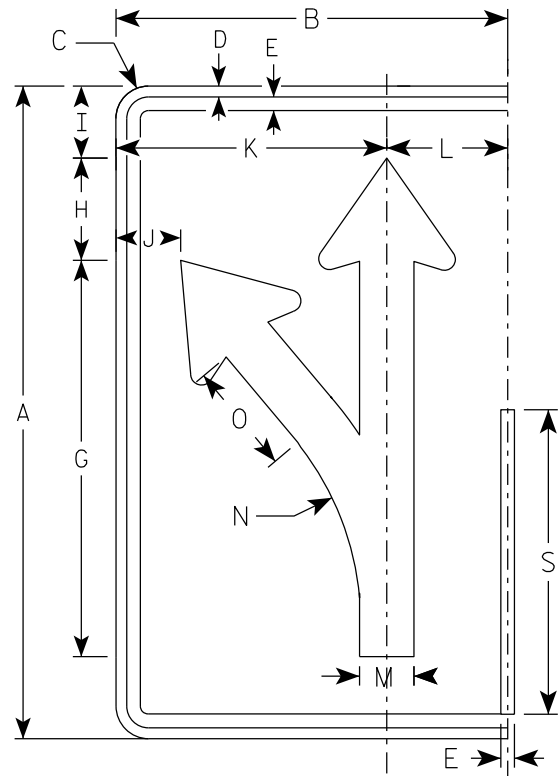
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

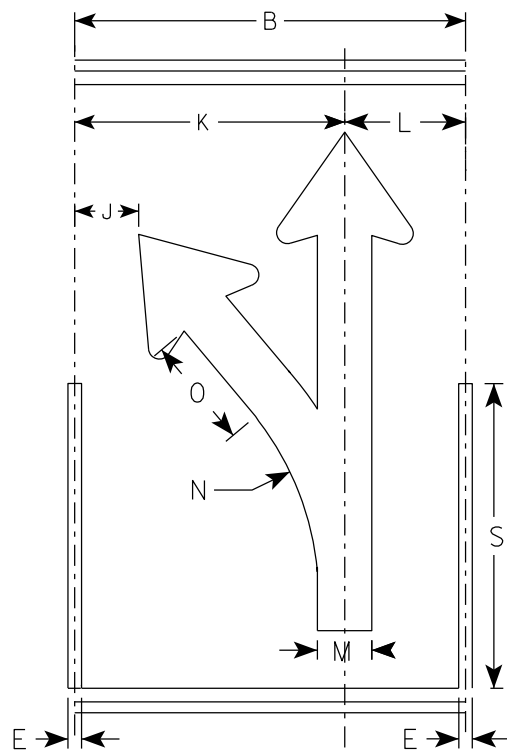
DATE 2/14/23 PLATE NO. R3-8.2

NOTES

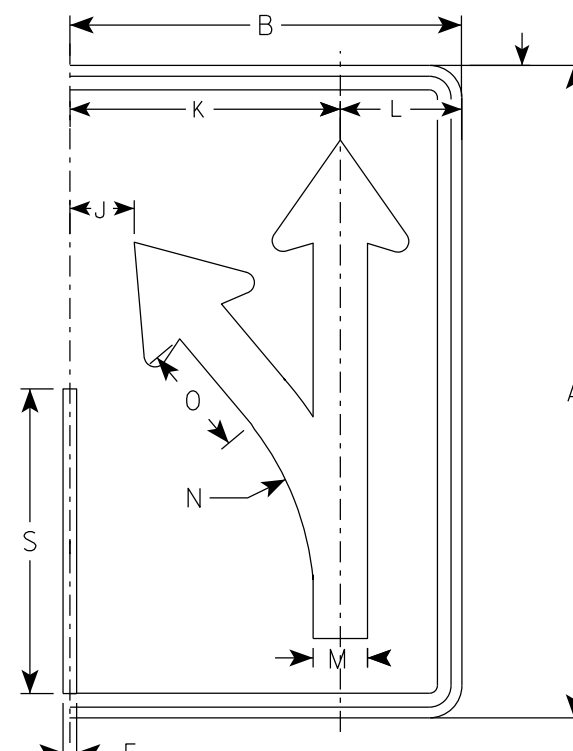
1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - None



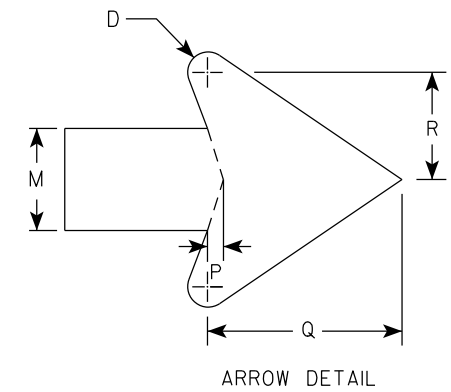
(E)



(E)



(E)



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30	18	1 1/2	1/2	5/8		18 1/4	4 3/4	3 1/4	3	12 1/2	5 1/2	2 1/2	13 1/4	5 1/8	3/8	4 3/4	2 5/8	14								3.75
2M	30	18	1 1/2	1/2	5/8		18 1/4	4 3/4	3 1/4	3	12 1/2	5 1/2	2 1/2	13 1/4	5 1/8	3/8	4 3/4	2 5/8	14								3.75
3	36	24	1 1/2	1/2	5/8		21 7/8	5 5/8	4	4 7/8	16 1/8	7 3/4	3	15 7/8	6 1/8	1/2	5 3/4	3 1/8	16 3/4								6.0
4	48	30	2 1/4	3/4	1		29 1/8	7 1/2	5 1/4	5 3/8	20 1/2	9 1/2	4	21 1/4	8 1/4	5/8	7 5/8	4 1/4	22 3/8								10.0
5	48	30	2 1/4	3/4	1		29 1/8	7 1/2	5 1/4	5 3/8	20 1/2	9 1/2	4	21 1/4	8 1/4	5/8	7 5/8	4 1/4	22 3/8								10.0

STANDARD SIGN
R3-8 (E) Arrow

WISCONSIN DEPT OF TRANSPORTATION

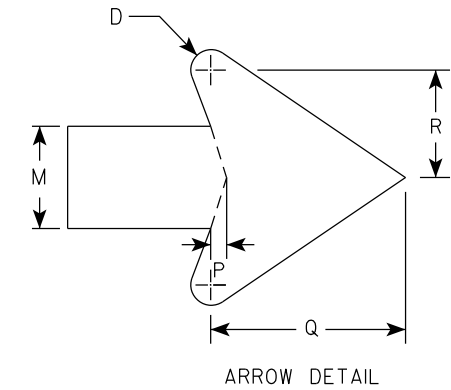
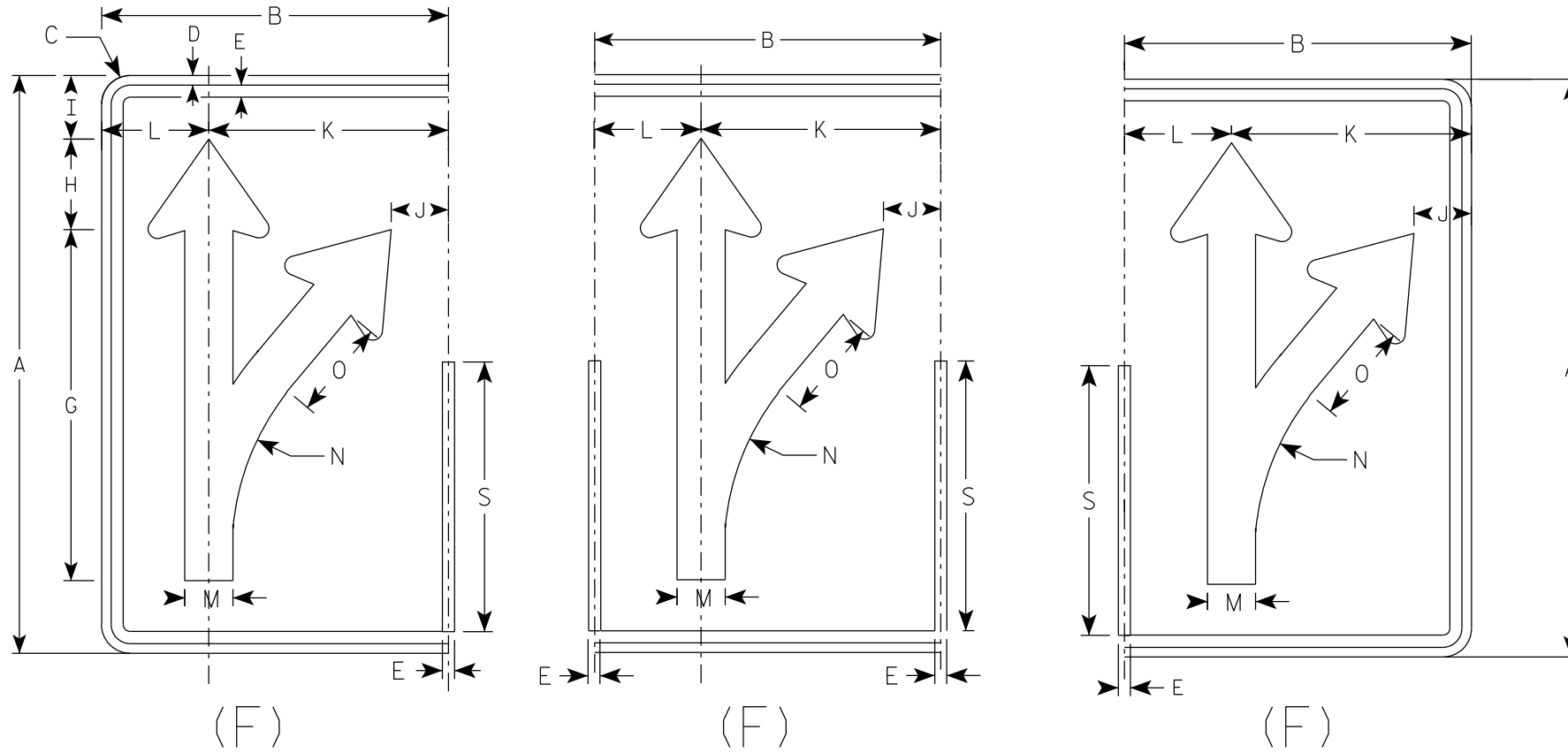
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 2/14/23 PLATE NO. R3-8.2

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - None



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30	18	1 1/2	1/2	5/8		18 1/4	4 3/4	3 1/4	3	12 1/2	5 1/2	2 1/2	13 1/4	5 1/8	3/8	4 3/4	2 5/8	14								3.75
2M	30	18	1 1/2	1/2	5/8		18 1/4	4 3/4	3 1/4	3	12 1/2	5 1/2	2 1/2	13 1/4	5 1/8	3/8	4 3/4	2 5/8	14								3.75
3	36	24	1 1/2	1/2	5/8		21 7/8	5 5/8	4	4 7/8	16 1/8	7 3/4	3	15 7/8	6 1/8	1/2	5 3/4	3 1/8	16 3/4								6.0
4	48	30	2 1/4	3/4	1		29 1/8	7 1/2	5 1/4	5 3/8	20 1/2	9 1/2	4	21 1/4	8 1/4	5/8	7 5/8	4 1/4	22 3/8								10.0
5	48	30	2 1/4	3/4	1		29 1/8	7 1/2	5 1/4	5 3/8	20 1/2	9 1/2	4	21 1/4	8 1/4	5/8	7 5/8	4 1/4	22 3/8								10.0

STANDARD SIGN
R3-8 (F) Arrow

WISCONSIN DEPT OF TRANSPORTATION

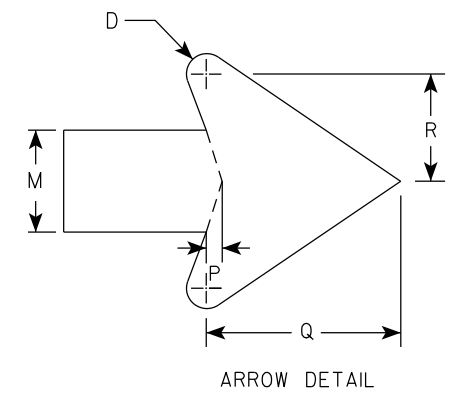
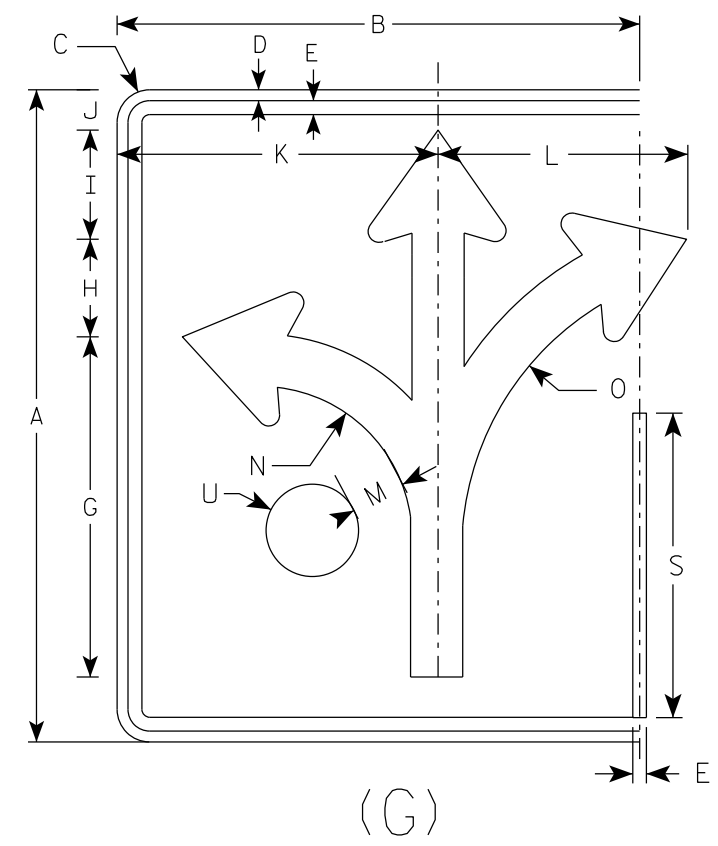
APPROVED *Matthew R Rauch*
for State Traffic Engineer

DATE 2/14/23 PLATE NO. R3-8.2

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - None



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30	24	1 1/2	1/2	5/8		15 5/8	4 1/2	5	1 7/8	14 3/4	11 1/2	2 3/8	7	13 1/4	3/8	4 1/2	2 1/2	14		2 1/8						5.0
2M	30	24	1 1/2	1/2	5/8		15 5/8	4 1/2	5	1 7/8	14 3/4	11 1/2	2 3/8	7	13 1/4	3/8	4 1/2	2 1/2	14		2 1/8						5.0
3	36	30	1 1/2	1/2	5/8		18 3/4	5 1/2	6	3 1/8	17 1/4	17 1/4	2 7/8	8 3/8	16	1/2	5 1/2	3	16 3/4		2 1/2						7.5
4	48	36	2 1/4	3/4	1		24 7/8	7 1/4	7 7/8	3 1/8	23 1/8	18	3 3/4	11 1/8	21 1/4	5/8	7 1/8	4	22 3/8		3 3/8						12.0
5	48	36	2 1/4	3/4	1		24 7/8	7 1/4	7 7/8	3 1/8	23 1/8	18	3 3/4	11 1/8	21 1/4	5/8	7 1/8	4	22 3/8		3 3/8						12.0

STANDARD SIGN
R3-8 (G) Arrow

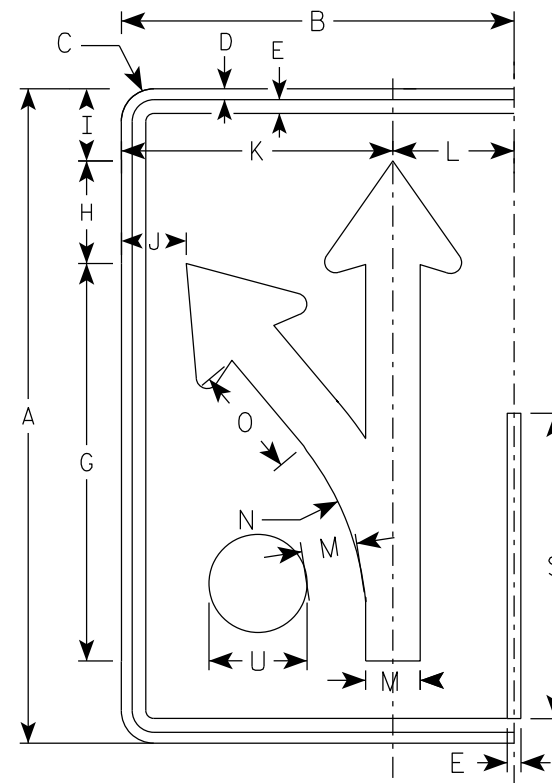
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

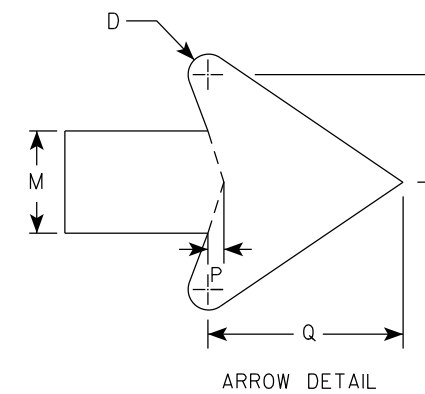
DATE 2/14/23 PLATE NO. R3-8.2

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - None



(H)



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30	18	1 1/2	1/2	5/8		18 1/4	4 3/4	3 1/4	3	12 1/2	5 1/2	2 1/2	13 1/4	5 1/8	3/8	4 3/4	2 5/8	14								3.75
2M	30	18	1 1/2	1/2	5/8		18 1/4	4 3/4	3 1/4	3	12 1/2	5 1/2	2 1/2	13 1/4	5 1/8	3/8	4 3/4	2 5/8	14								3.75
3	36	24	1 1/2	1/2	5/8		21 7/8	5 5/8	4	4 7/8	16 1/8	7 3/4	3	15 7/8	6 1/8	1/2	5 3/4	3 1/8	16 3/4								6.0
4	48	30	2 1/4	3/4	1		29 1/8	7 1/2	5 1/4	5 3/8	20 1/2	9 1/2	4	21 1/4	8 1/4	5/8	7 5/8	4 1/4	22 3/8								10.0
5	48	30	2 1/4	3/4	1		29 1/8	7 1/2	5 1/4	5 3/8	20 1/2	9 1/2	4	21 1/4	8 1/4	5/8	7 5/8	4 1/4	22 3/8								10.0

STANDARD SIGN
R3-8 (H) Arrow

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 2/14/23 PLATE NO. R3-8.2

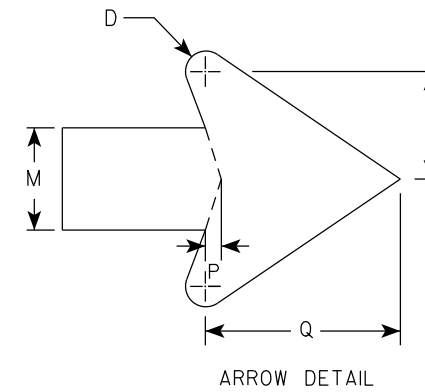
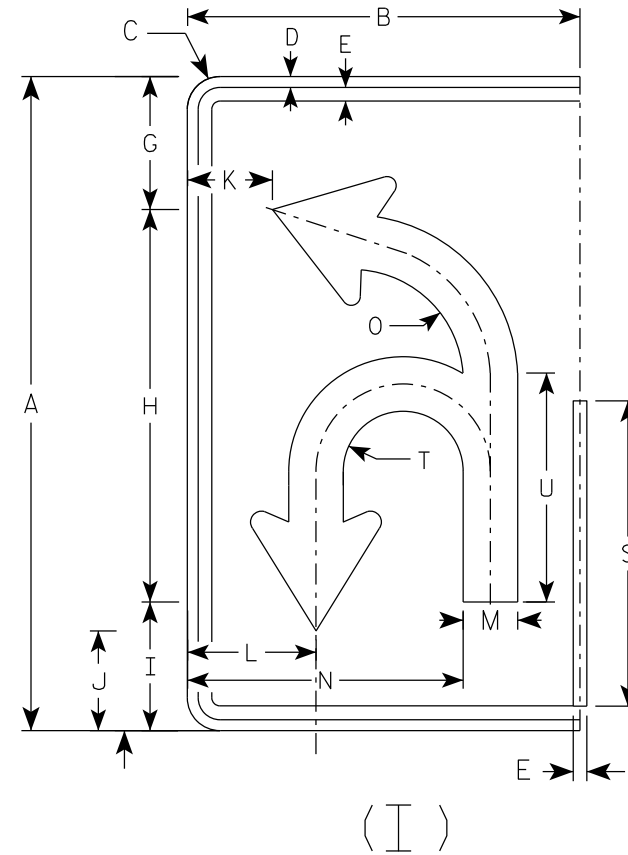
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

7

7

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - None



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30	18	1 1/2	1/2	5/8		6 1/8	18	5 7/8	4 5/8	3 7/8	5 7/8	2 1/2	12 5/8	5 1/8	3/8	4 3/4	2 5/8	14	2 3/4	10 1/2						3.75
2M	30	18	1 1/2	1/2	5/8		6 1/8	18	5 7/8	4 5/8	3 7/8	5 7/8	2 1/2	12 5/8	5 1/8	3/8	4 3/4	2 5/8	14	2 3/4	10 1/2						3.75
3	36	24	1 1/2	1/2	5/8		21 7/8	21 5/8	7 1/8	5 1/2	5 7/8	8 1/4	3	16 3/8	6 1/8	1/2	5 3/4	3 1/8	16 3/4	3 1/4	12 5/8						6.0
4	48	30	2 1/4	3/4	1		29 1/8	28 3/4	9 3/8	7 1/4	6 7/8	10	4	20 7/8	8 1/8	5/8	7 5/8	4 1/4	22 3/8	4 3/8	16 3/4						10.0
5	48	30	2 1/4	3/4	1		29 1/8	28 3/4	9 3/8	7 1/4	6 7/8	10	4	20 7/8	8 1/8	5/8	7 5/8	4 1/4	22 3/8	4 3/8	16 3/4						10.0

STANDARD SIGN
R3-8 (I) Arrow

WISCONSIN DEPT OF TRANSPORTATION

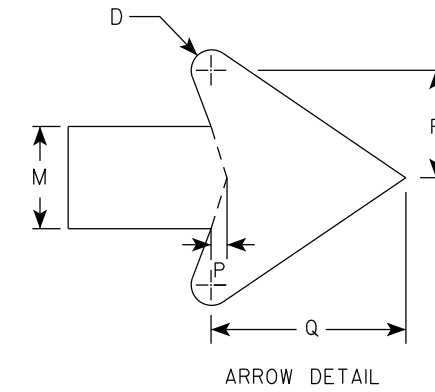
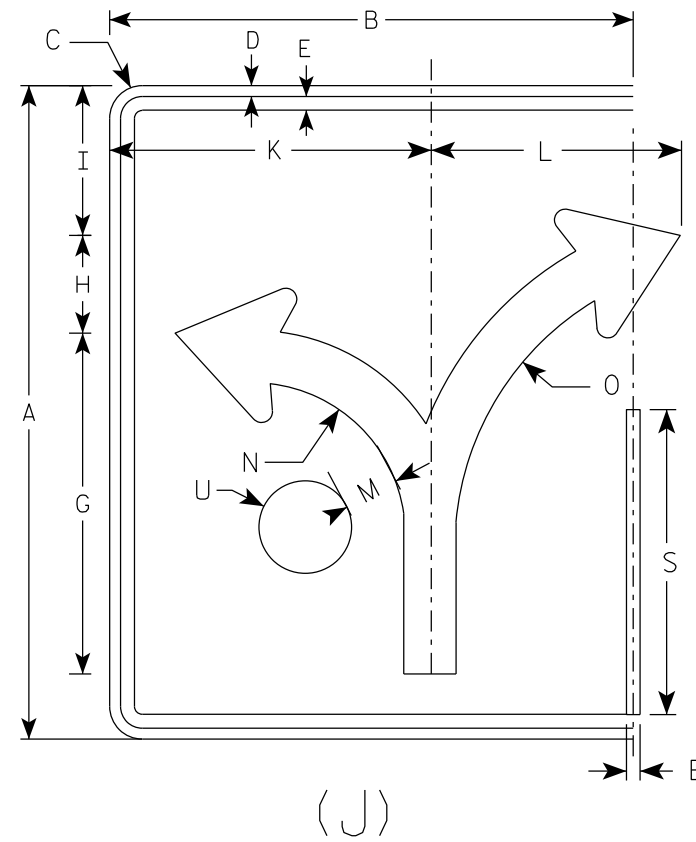
APPROVED *Matthew R Rauch*
for State Traffic Engineer

DATE 2/14/23 PLATE NO. R3-8.2

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - None



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30	24	1 1/2	1/2	5/8		15 5/8	4 1/2	6 7/8		14 3/4	11 1/2	2 3/8	7	13 1/4	3/8	4 1/2	2 1/2	14		2 1/8						5.0
2M	30	24	1 1/2	1/2	5/8		15 5/8	4 1/2	6 7/8		14 3/4	11 1/2	2 3/8	7	13 1/4	3/8	4 1/2	2 1/2	14		2 1/8						5.0
3	36	30	1 1/2	1/2	5/8		18 3/4	5 1/2	8 1/4		17 1/4	17 1/4	2 7/8	8 3/8	16	1/2	5 1/2	3	16 3/4		2 1/2						7.5
4	48	36	2 1/4	3/4	1		24 7/8	7 1/4	11		23 1/8	18	3 3/4	11 1/8	21 1/4	5/8	7 1/8	4	22 3/8		3 3/8						12.0
5	48	36	2 1/4	3/4	1		24 7/8	7 1/4	11		23 1/8	18	3 3/4	11 1/8	21 1/4	5/8	7 1/8	4	22 3/8		3 3/8						12.0

STANDARD SIGN
R3-8 (J) Arrow

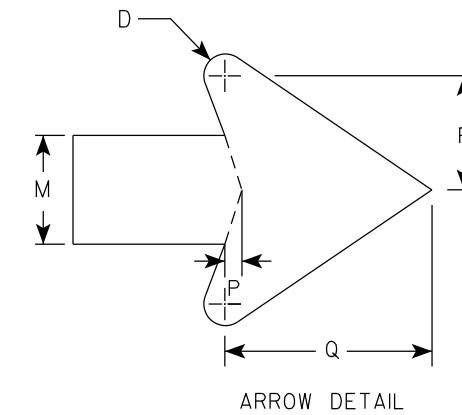
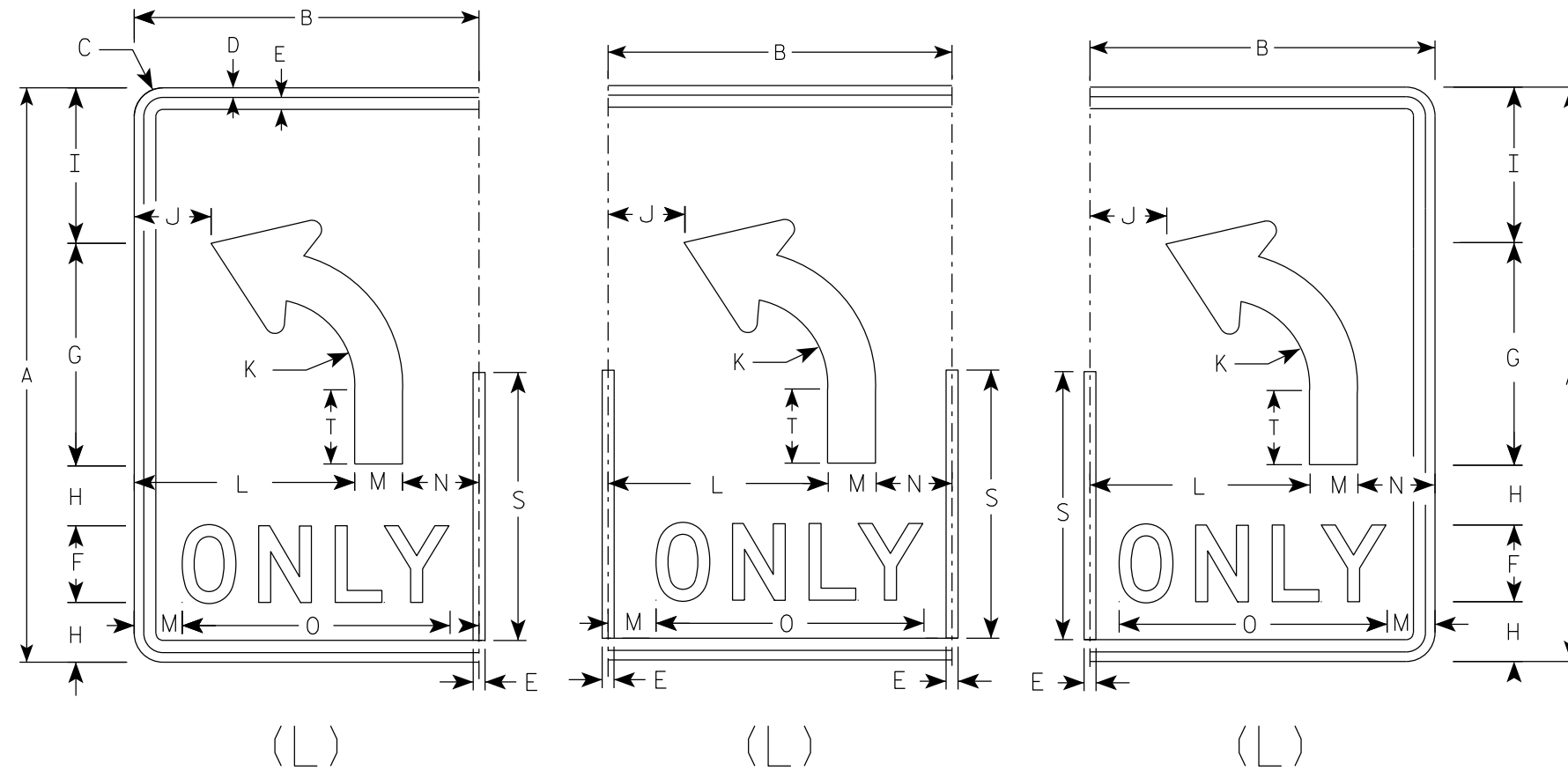
WISCONSIN DEPT OF TRANSPORTATION
APPROVED *Matthew R Rauch*
for State Traffic Engineer
DATE 2/14/23 PLATE NO. R3-8.2

7

7

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - D



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30	18	1 1/2	1/2	5/8	4	11 5/8	3 1/8	8 1/8	4	4 1/2	11 1/2	2 1/2	4	14	3/8	4 3/4	2 5/8	14	3 7/8							3.75
2M	30	18	1 1/2	1/2	5/8	4	11 5/8	3 1/8	8 1/8	4	4 1/2	11 1/2	2 1/2	4	14	3/8	4 3/4	2 5/8	14	3 7/8							3.75
3	36	24	1 1/2	1/2	5/8	5	14	3 1/2	9 3/4	6	5 3/8	15	3	6	17 5/8	1/2	5 3/4	3 1/8	16 3/4	4 5/8							6.0
4	48	30	2 1/4	3/4	1	6	18 5/8	5 1/8	13 1/8	6 1/8	7 1/4	18	4	8	21 3/4	5/8	7 5/8	4 1/4	22 3/8	6 1/4							10.0
5	48	30	2 1/4	3/4	1	6	18 5/8	5 1/8	13 1/8	6 1/8	7 1/4	18	4	8	21 3/4	5/8	7 5/8	4 1/4	22 3/8	6 1/4							10.0

STANDARD SIGN
R3-8 (L) Arrow

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 2/14/23 PLATE NO. R3-8.2

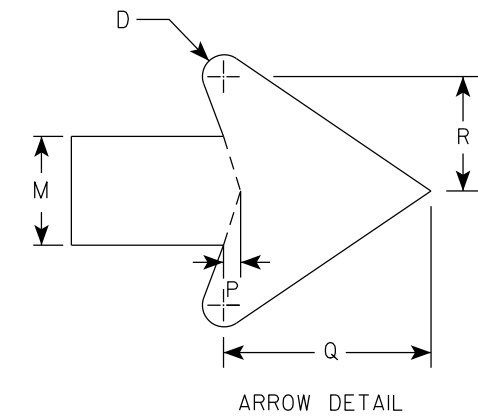
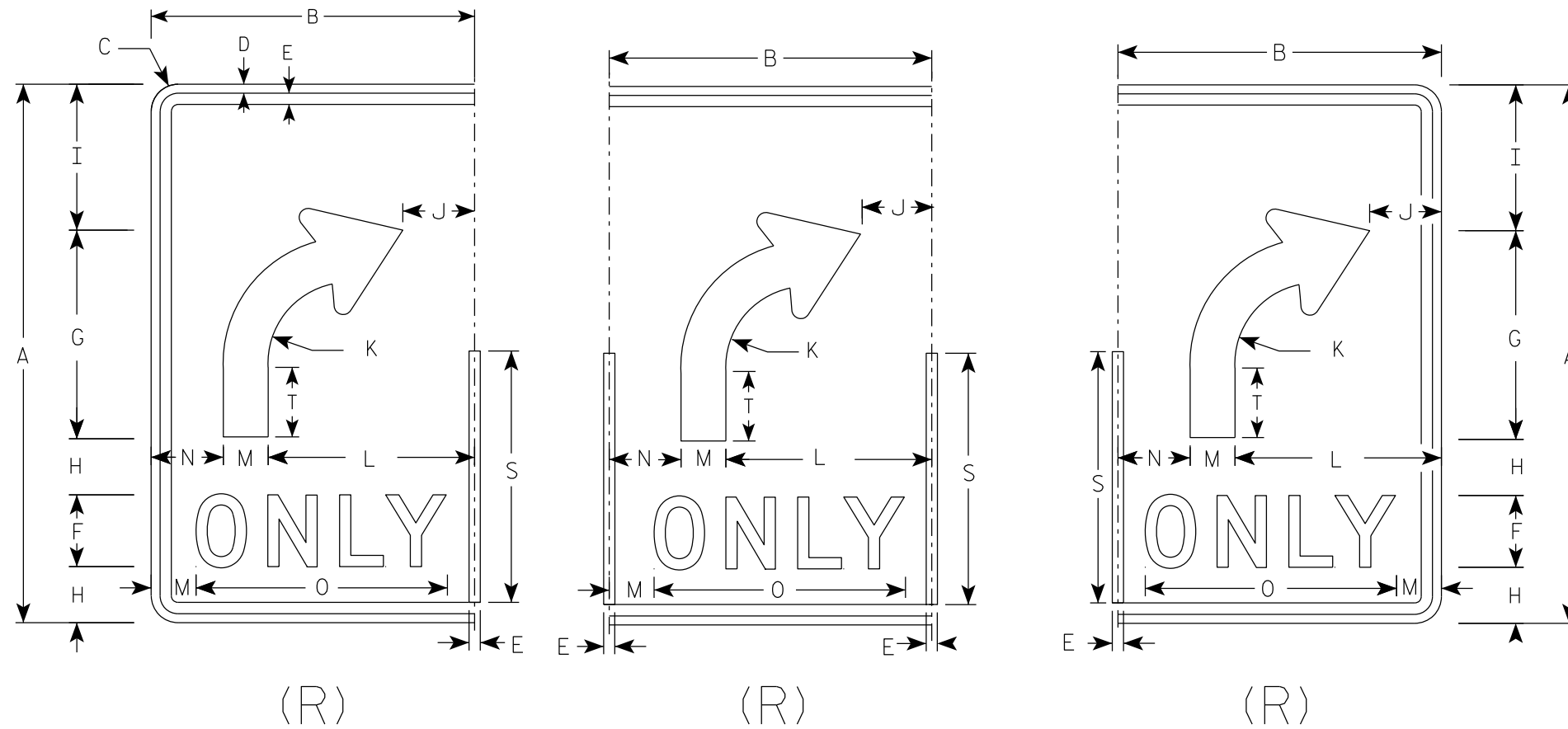
PROJECT NO:

SHEET NO:

E

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - D



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30	18	1 1/2	1/2	5/8	4	11 5/8	3 1/8	8 1/8	4	4 1/2	11 1/2	2 1/2	4	14	3/8	4 3/4	2 5/8	14	3 7/8							3.75
2M	30	18	1 1/2	1/2	5/8	4	11 5/8	3 1/8	8 1/8	4	4 1/2	11 1/2	2 1/2	4	14	3/8	4 3/4	2 5/8	14	3 7/8							3.75
3	36	24	1 1/2	1/2	5/8	5	14	3 1/2	9 3/4	6	5 3/8	15	3	6	17 5/8	1/2	5 3/4	3 1/8	16 3/4	4 5/8							6.0
4	48	30	2 1/4	3/4	1	6	18 5/8	5 1/8	13 1/8	6 1/8	7 1/4	18	4	8	21 3/4	5/8	7 5/8	4 1/4	22 3/8	6 1/4							10.0
5	48	30	2 1/4	3/4	1	6	18 5/8	5 1/8	13 1/8	6 1/8	7 1/4	18	4	8	21 3/4	5/8	7 5/8	4 1/4	22 3/8	6 1/4							10.0

STANDARD SIGN
R3-8 (R) Arrow

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

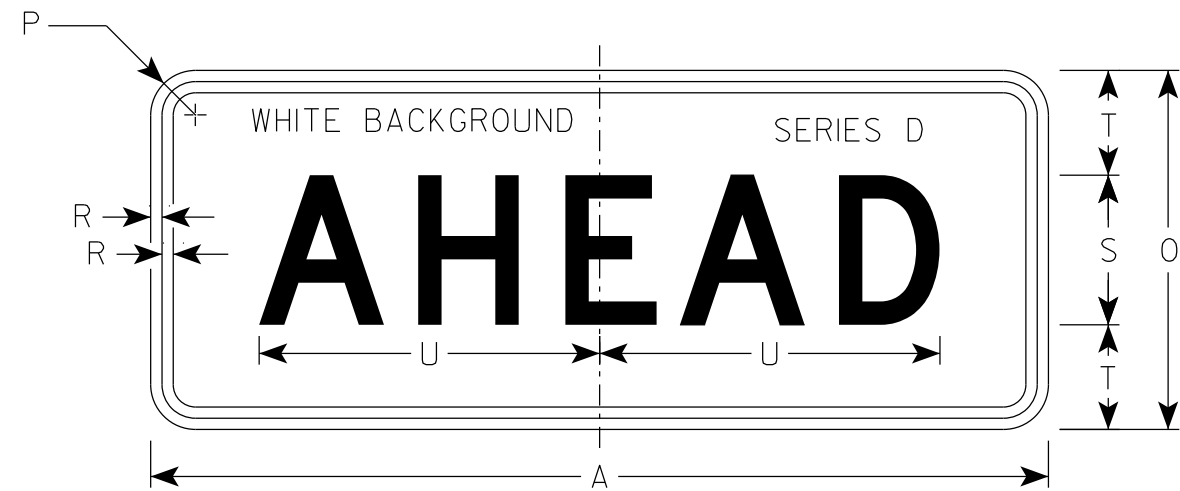
DATE 2/14/23 PLATE NO. R3-8.2

NOTES

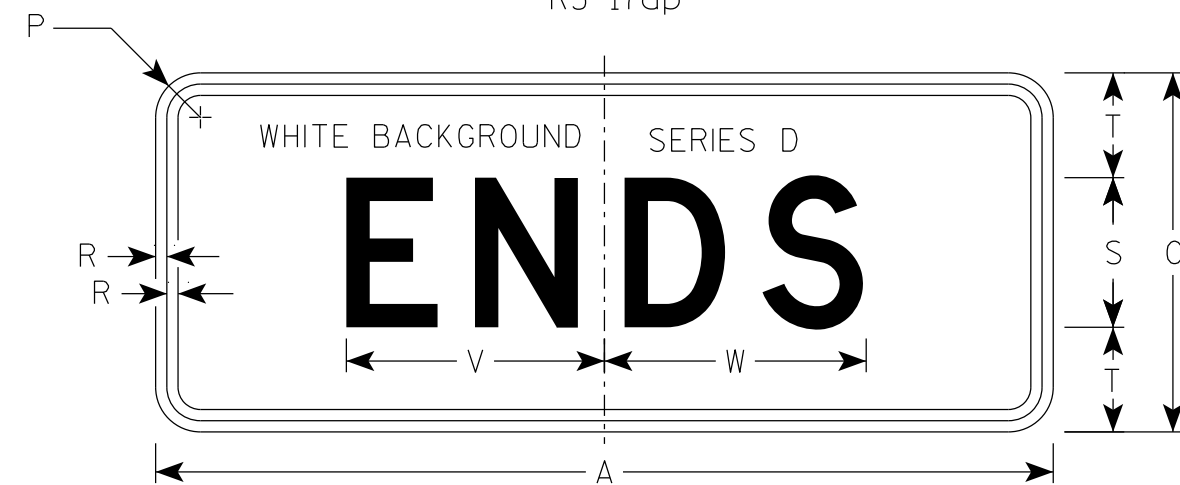
1. Sign is Type II - Type H Reflective
2. Color:
Background - AS SHOWN
Message - BLACK
3. Message Series - C or as noted



R3-17



R3-17ap



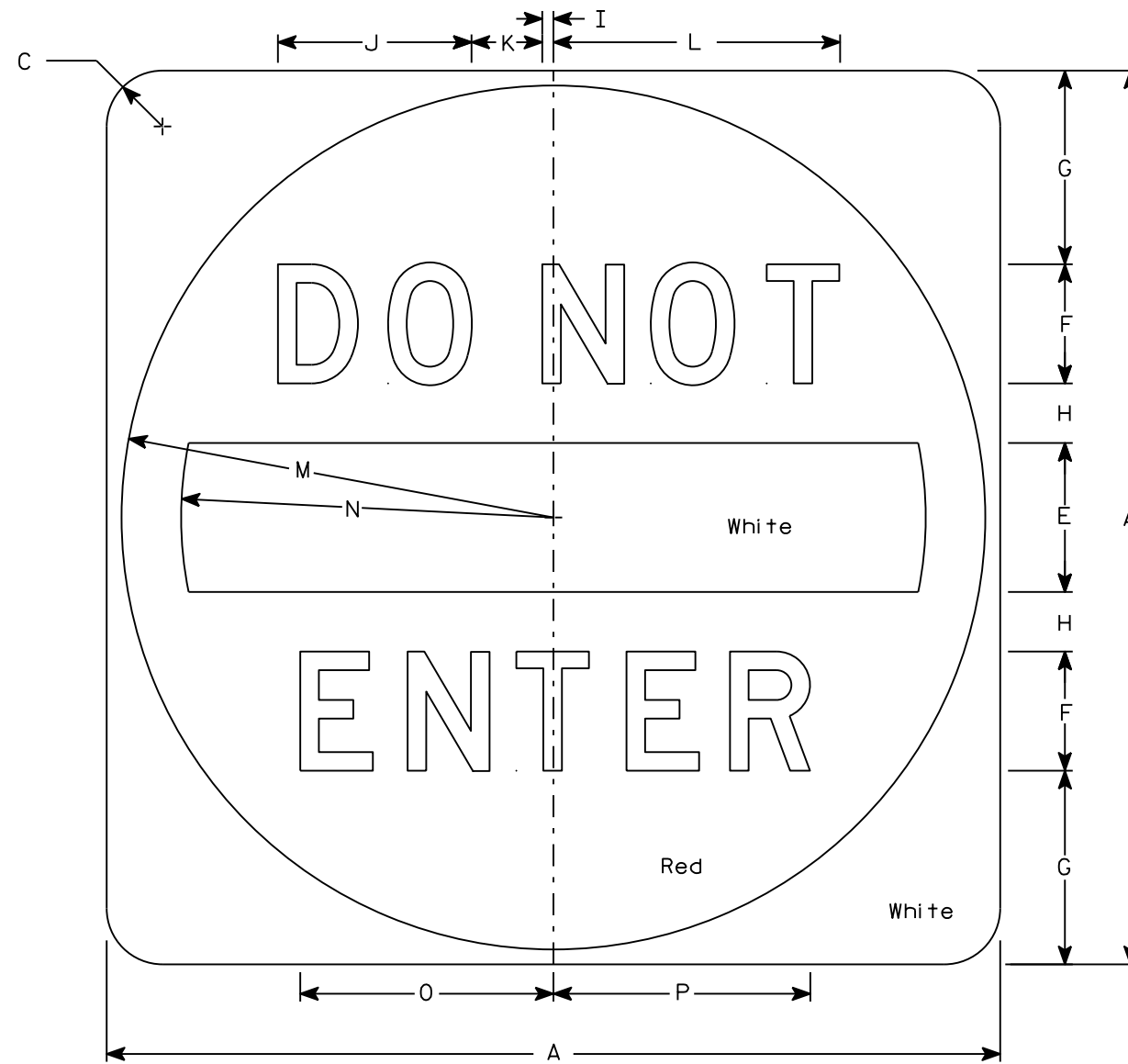
R3-17bp

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	R3-17 Area sq. ft.	R3-17ap Area sq. ft.	R3-17bp Area sq. ft.
1																													
2S	30	24	1 1/2	3/8	1/2	2	4	4 1/8	7 7/8	6 3/8	9 1/2	2 5/8	7/8	13	12	1 1/2	3 3/8	3/8	5	3 1/2	11 3/8	8 5/8	8 3/4	2 3/8	15 5/8	8	5.0	2.5	2.5
2M	30	24	1 1/2	3/8	1/2	2	4	4 1/8	7 7/8	6 3/8	9 1/2	2 5/8	7/8	13	12	1 1/2	3 3/8	3/8	5	3 1/2	11 3/8	8 5/8	8 3/4	2 3/8	15 5/8	8	5.0	2.5	2.5
3																													
4																													
5																													

STANDARD SIGN
R3-17 & R3-17a&bp
WISCONSIN DEPT OF TRANSPORTATION
APPROVED *Matthew R Raub*
for State Traffic Engineer
DATE 2/23/23 PLATE NO. R3-17.3

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - See detail
Message - White
3. Message Series - D



R5-1

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30		1 7/8		5	4	6 1/2	2	3/8	6 1/2	2 3/8	9 5/8	14 1/2	12 1/2	8 1/2	8 5/8											6.25
2M	36		2 1/4		6	5	7 1/2	2 1/2	1/2	8 1/8	3	12 1/8	17 1/2	15	10 5/8	10 3/4											9.0
3	36		2 1/4		6	5	7 1/2	2 1/2	1/2	8 1/8	3	12 1/8	17 1/2	15	10 5/8	10 3/4											9.0
4	36		2 1/4		6	5	7 1/2	2 1/2	1/2	8 1/8	3	12 1/8	17 1/2	15	10 5/8	10 3/4											9.0
5	48		3		8	6	11	3	5/8	9 3/4	3 5/8	14 1/2	23 1/2	20	12 3/4	12 7/8											16.0

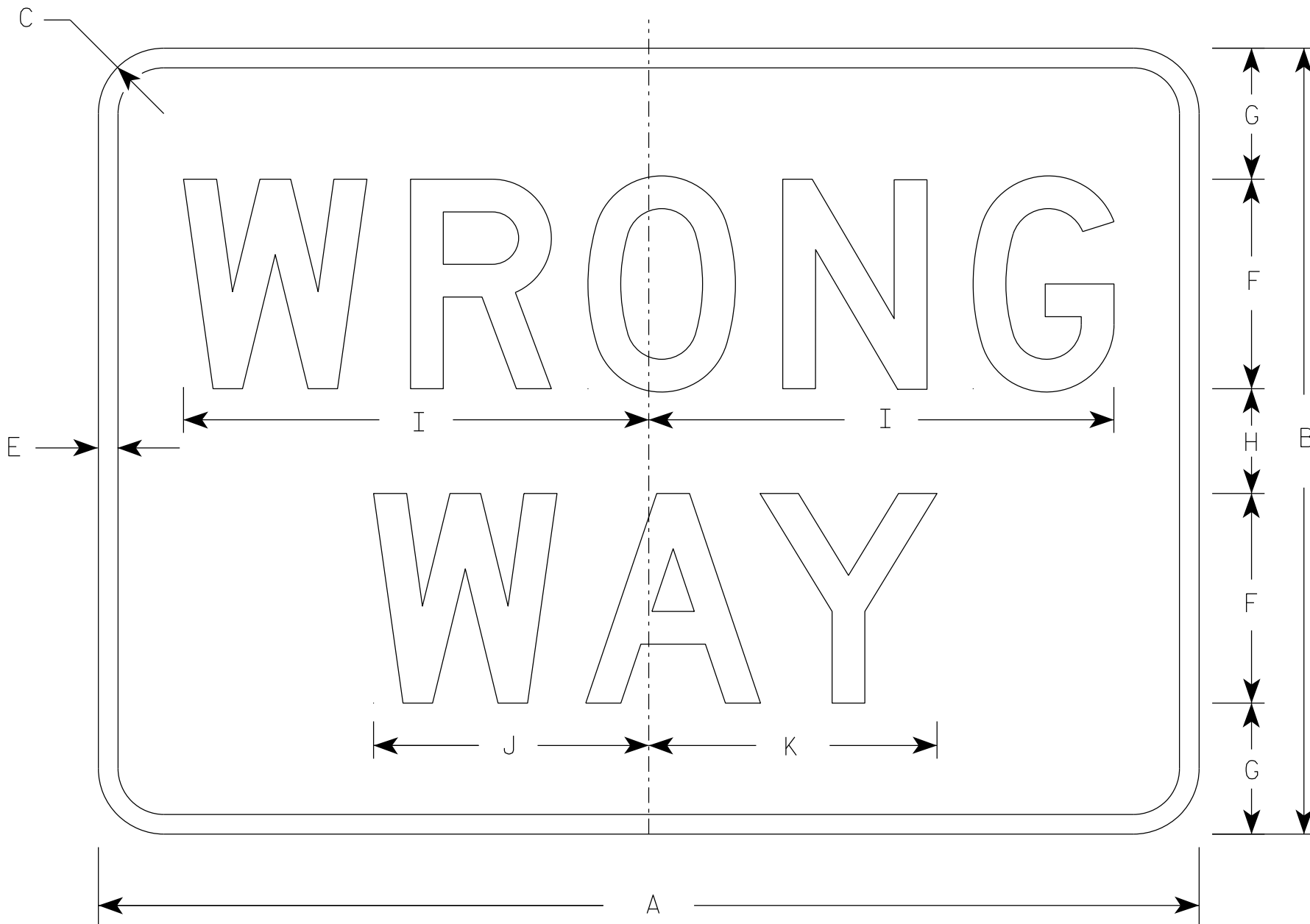
STANDARD SIGN
R5-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 3/15/18 PLATE NO. R5-1.16

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**



NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - Red
Message - White
3. Message Series - D

7

7

R5-1A

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30	18	1 1/2		1/2	5	3	2	11	6 1/2	6 7/8																3.75
2S	36	24	1 1/2		5/8	6	4 1/2	3	13 1/4	7 7/8	8 1/4																6.00
2M	42	30	1 7/8		3/4	8	5	4	17 3/4	10 1/2	11																8.75
3	42	30	1 7/8		3/4	8	5	4	17 3/4	10 1/2	11																8.75
4	42	30	1 7/8		3/4	8	5	4	17 3/4	10 1/2	11																8.75
5	42	30	1 7/8		3/4	8	5	4	17 3/4	10 1/2	11																8.75

STANDARD SIGN
R5-1A

WISCONSIN DEPT OF TRANSPORTATION

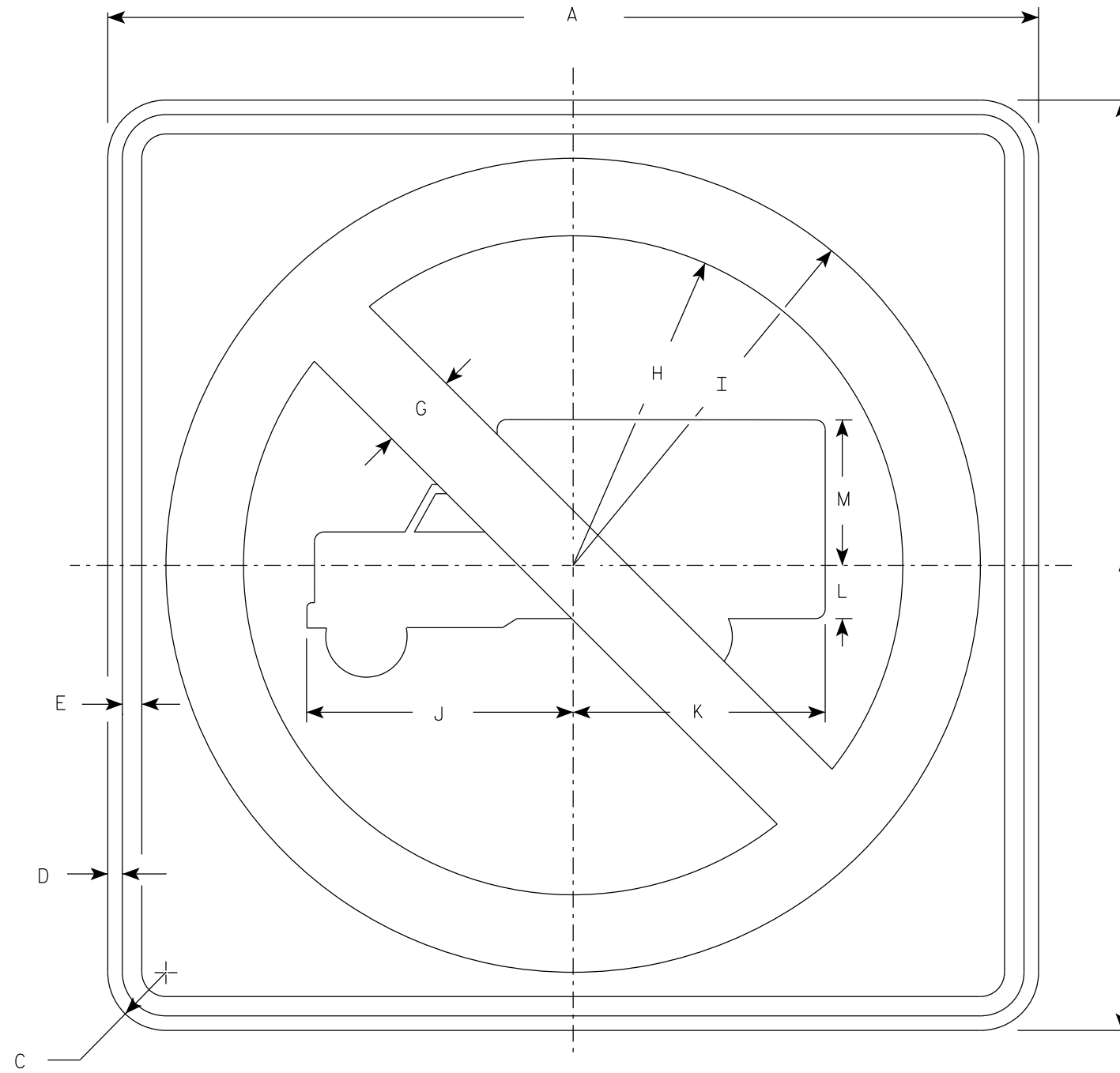
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 8/17/23 PLATE NO. R5-1A.3

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
 - Background - White
 - Message - See Note 3
3. Circle & Diagonal - Red
Truck Symbol & Border - Black



R5-2

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24		1 1/2	3/8	1/2		2	8 1/2	10 1/2	6 7/8	6 1/2	1 3/8	3 3/4														4.0
2M	24		1 1/2	3/8	1/2		2	8 1/2	10 1/2	6 7/8	6 1/2	1 3/8	3 3/4														4.0
3	30		1 7/8	1/2	5/8		2 1/2	10 5/8	13 1/8	8 1/2	8 1/8	1 5/8	4 3/4														6.25
4	36		2 1/4	5/8	3/4		3	12 3/4	15 3/4	10 1/4	9 3/4	2	5 3/4														9.0
5	48		2 1/4	3/4	1		4	17	21	13 5/8	13	2 5/8	7 5/8														16.0

STANDARD SIGN
R5-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 8/17/23 PLATE NO. R5-2.7

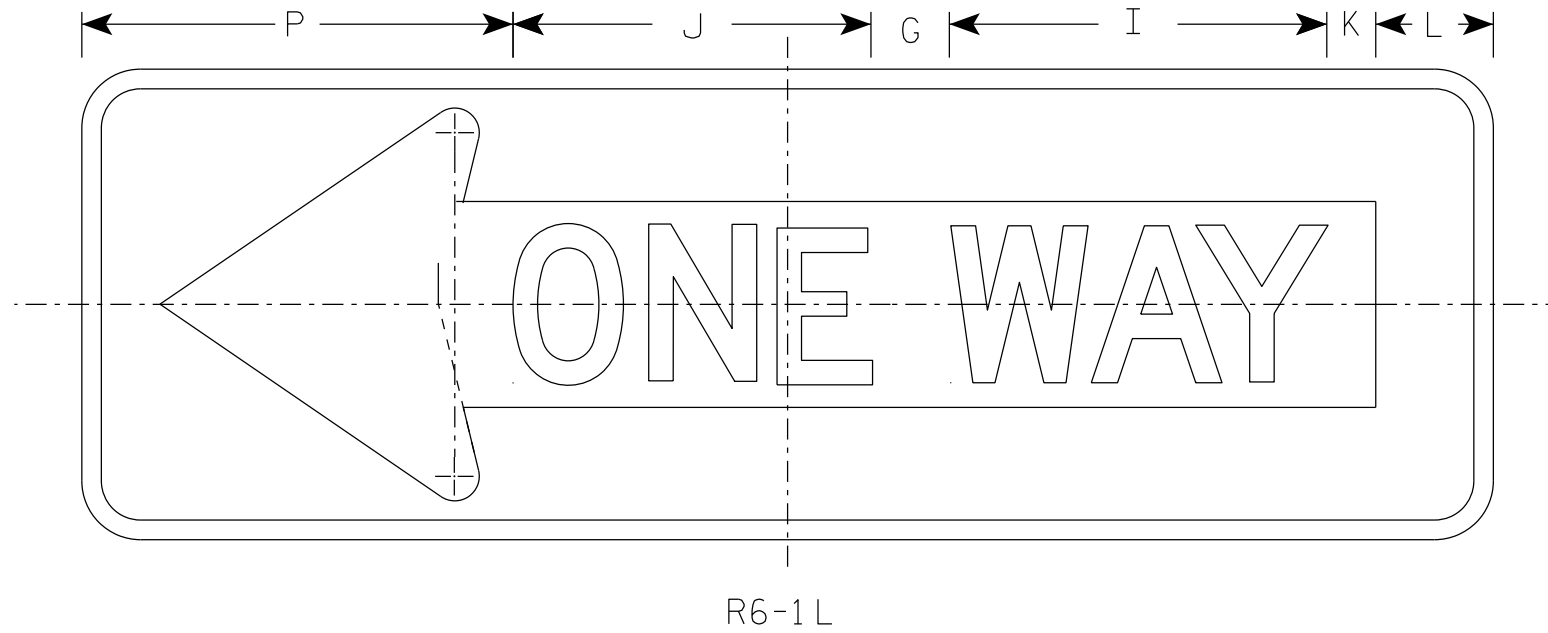
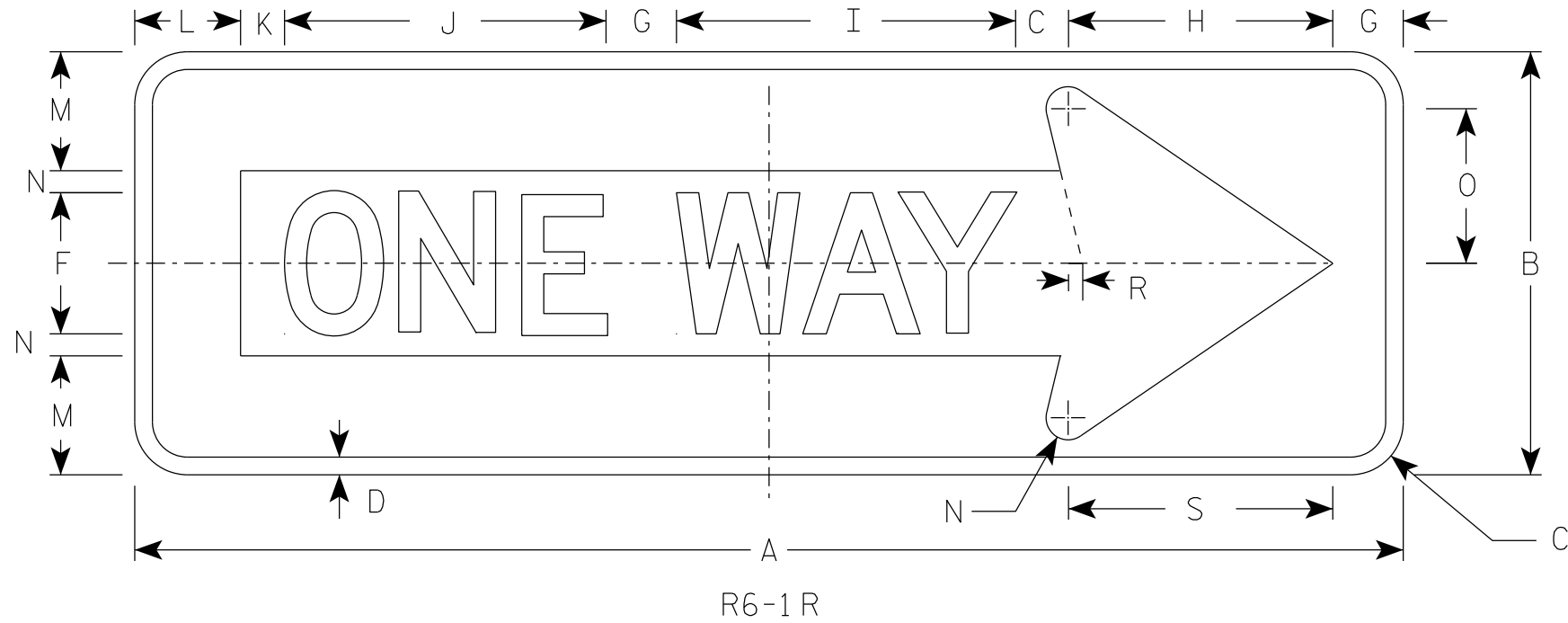
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

7

7

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - Black
Message - Black Legend & White Arrow & Border
3. Message Series - D



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	36	12	1 1/2	1/2		4	2	7 1/2	9 5/8	9 1/8	1 1/4	3	3 3/8	5/8	4 3/8	11		3/8	7 1/2								3.0
2M	54	18	1 1/2	3/4		6	3	11 1/4	13 5/8	14 1/2	1 7/8	4 1/2	5	1	6 1/2	16 1/2		5/8	11 1/4								6.75
3	54	18	1 1/2	3/4		6	3	11 1/4	13 5/8	14 1/2	1 7/8	4 1/2	5	1	6 1/2	16 1/2		5/8	11 1/4								6.75
4	54	18	1 1/2	3/4		6	3	11 1/4	13 5/8	14 1/2	1 7/8	4 1/2	5	1	6 1/2	16 1/2		5/8	11 1/4								6.75
5																											

STANDARD SIGN
R6-1 L&R

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

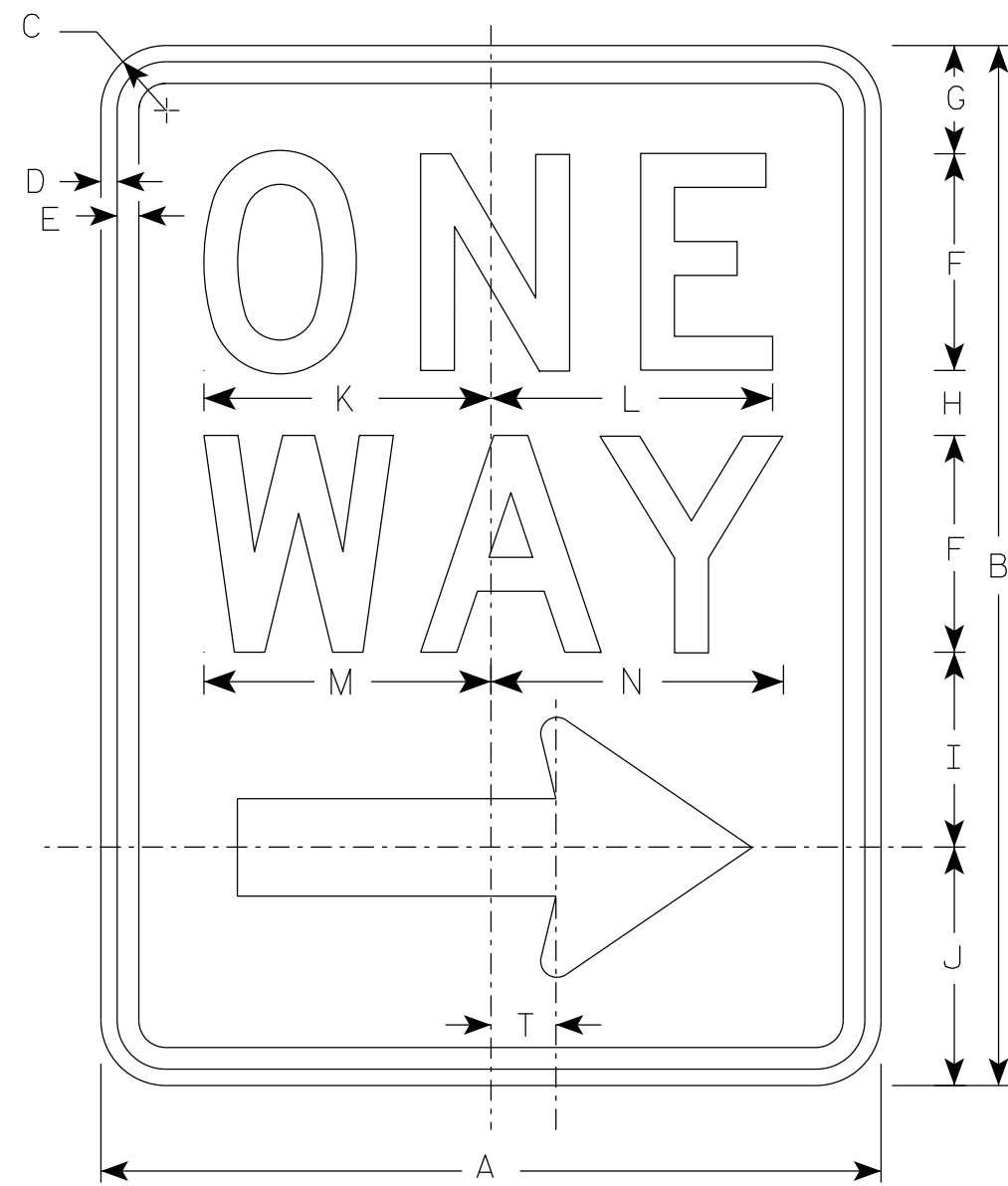
DATE 8/30/23 PLATE NO. R6-1.4

7

7

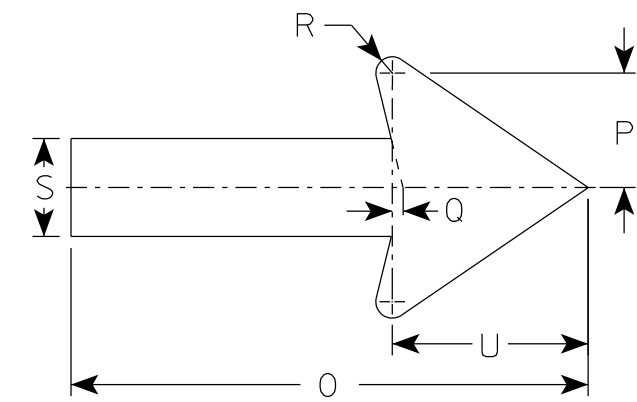
NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - D
4. R6-2L same as R6-2R except arrow points to the left.



R6-2R

Arrow Detail



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z
1	18	24	1 1/2	3/8	1/2	5	2 1/2	1 1/2	4 1/2	5 1/2	6 5/8	6 1/2	6 5/8	6 3/4	11 7/8	2 5/8	1/4	3/8	2 1/4	1 1/2	4 1/2					
2S	24	30	1 1/2	3/8	1/2	6	3	2 1/2	5 1/2	7	8 1/8	8 1/8	8 1/2	8 5/8	16	3 1/2	3/8	1/2	3	2	6					
2M	30	36	1 7/8	1/2	5/8	8	2 1/2	2 5/8	6 7/8	8	10 1/2	10 1/2	11 1/4	11 1/4	20	4 3/8	1/2	5/8	3 3/4	2 1/2	7 1/2					
3	36	48	1 7/8	1/2	5/8	10	5 1/4	3 1/4	9	10 1/2	12 3/4	12 3/4	13 1/4	13 1/2	24	5 5/8	1/2	3/4	4 3/4	3	9					
4	36	48	1 7/8	1/2	5/8	10	5 1/4	3 1/4	9	10 1/2	12 3/4	12 3/4	13 1/4	13 1/2	24	5 5/8	1/2	3/4	4 3/4	3	9					
5																										

STANDARD SIGN
R6-2 R&L

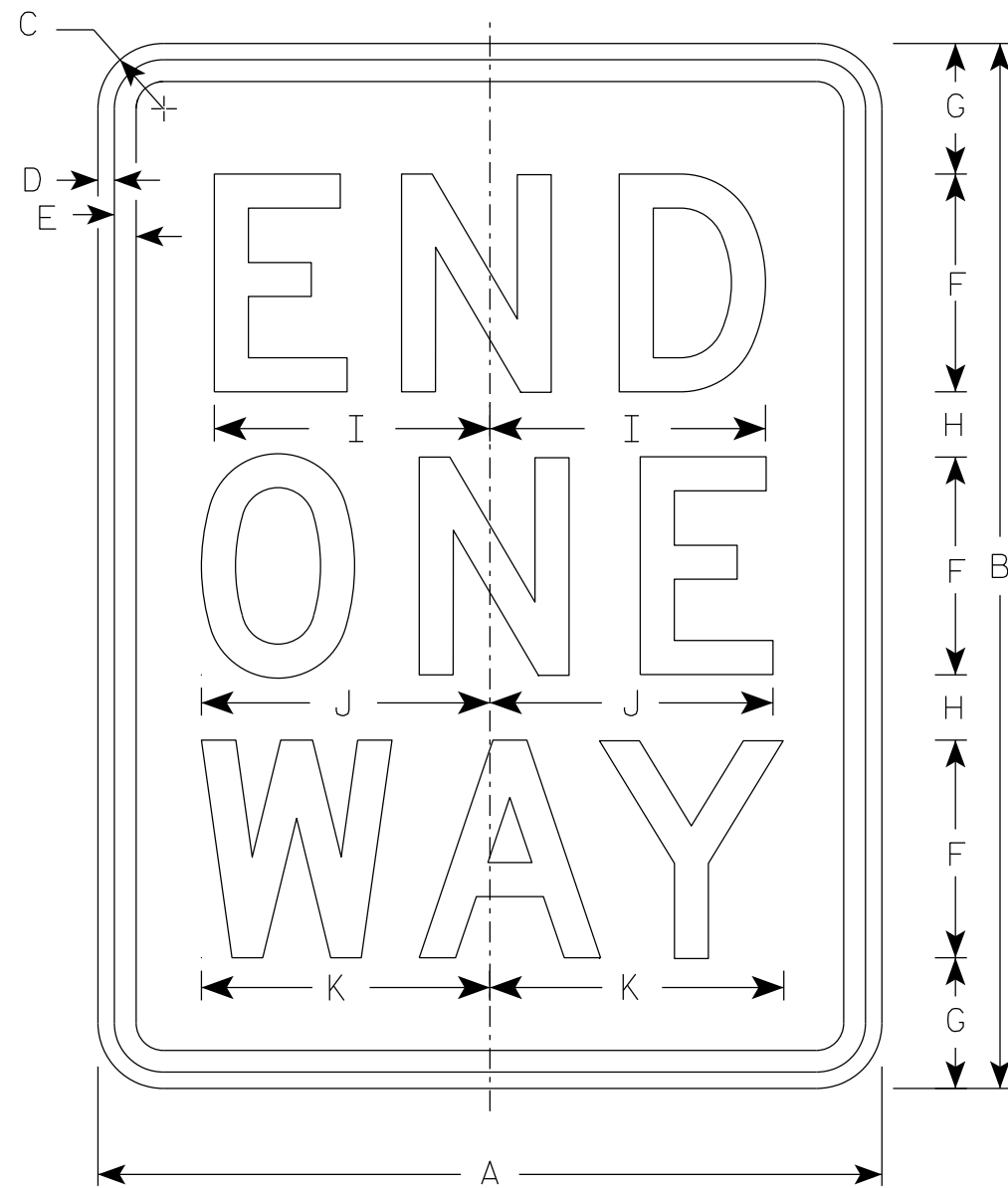
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 11/2/10 PLATE NO. R6-2.8

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - D



R6 - 7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z
1																										
2S	24	30	1 1/2	3/8	1/2	6	3 1/2	2 1/2	8	8 1/8	8 1/2															
2M	30	36	1 7/8	1/2	5/8	8	3 1/4	2 3/4	10 1/8	10 1/2	11 1/2															
3	36	48	1 7/8	1/2	5/8	10	5 1/4	3 1/4	12 5/8	12 3/4	13 3/8															
4	36	48	1 7/8	1/2	5/8	10	5 1/4	3 1/4	12 5/8	12 3/4	13 3/8															
5																										

STANDARD SIGN

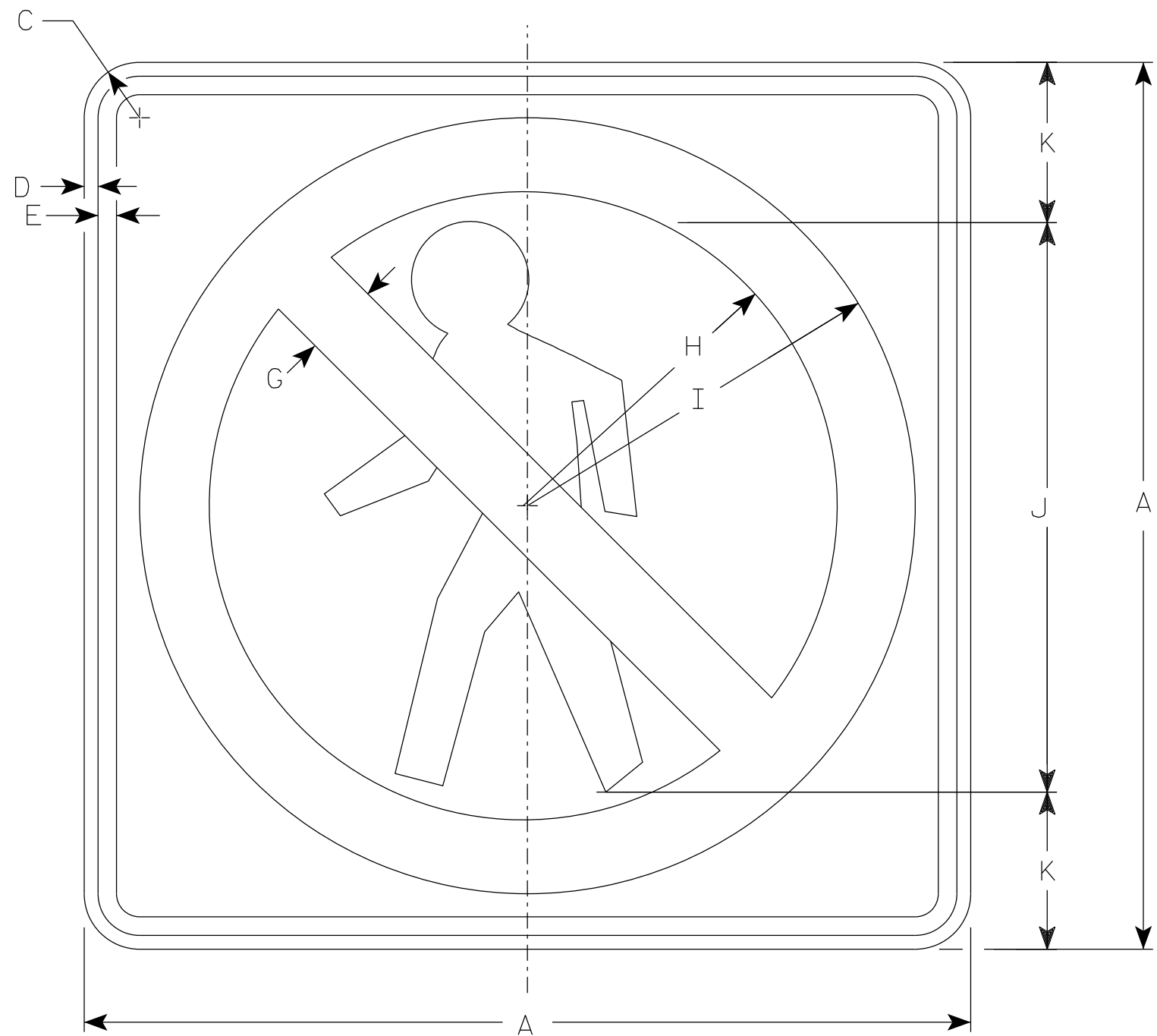
R6 - 7

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Raub*
for State Traffic Engineer

DATE 10/26/23 PLATE NO. R6-7.2

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**



R9-3A

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - See note 3
3. Border & Pedestrian are non-reflective black.
Circle with diagonal bar is reflective red.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18		1 1/2	3/8	3/8		1 1/2	6 1/4	7 7/8	12	3																2.25
2S	24		1 1/2	3/8	1/2		2	8 1/2	10 1/2	15	4																4.00
2M	24		1 1/2	3/8	1/2		2	8 1/2	10 1/2	15	4																4.00
3																											
4																											
5																											

STANDARD SIGN
R9-3A

WISCONSIN DEPT OF TRANSPORTATION

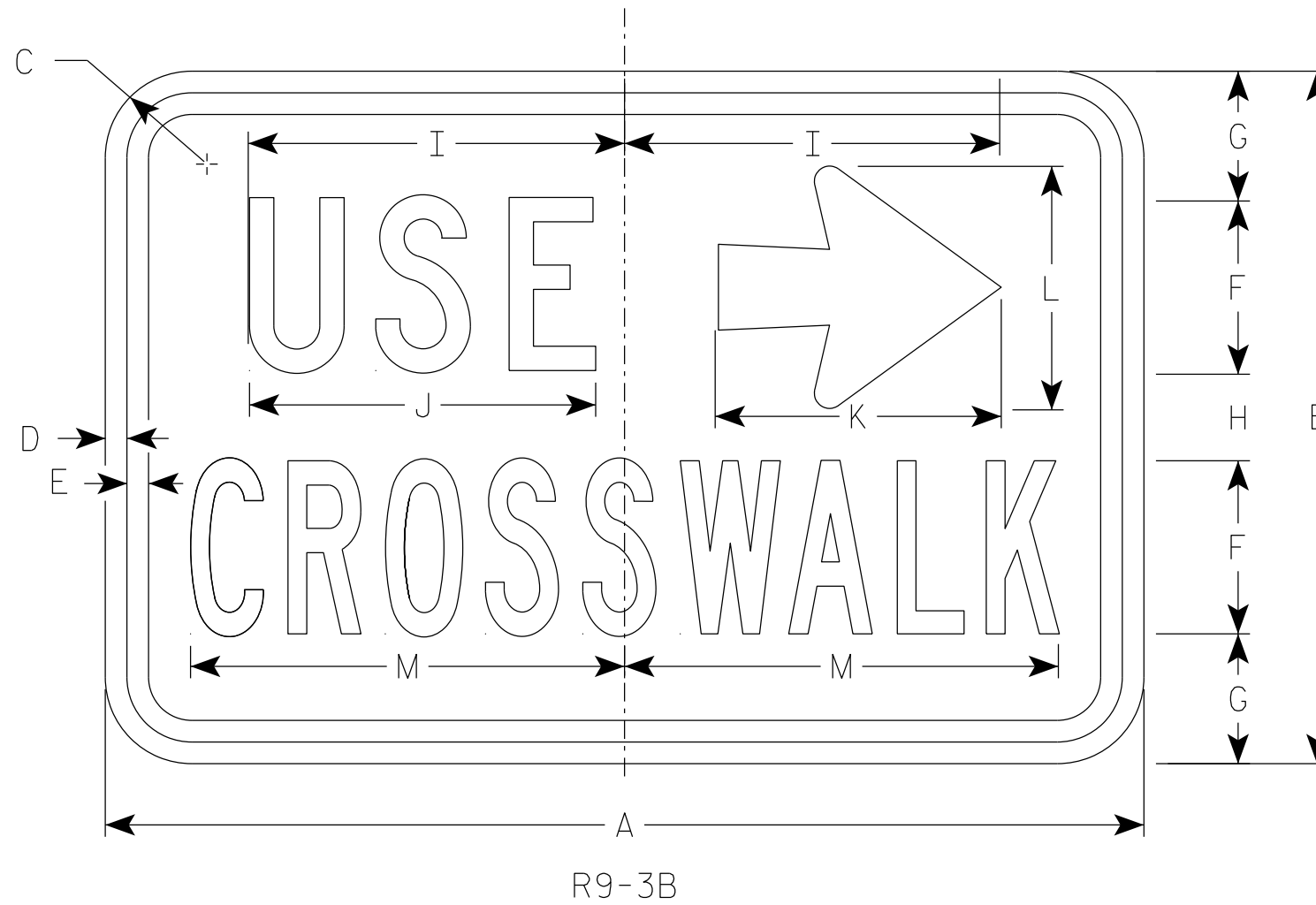
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 1/23/24 PLATE NO. R9-3A.4

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - C Line 1, Series B Line 2



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	18	12	1 1/2	3/8	3/8	3	2 1/4	1 1/2	6 1/2	6	5	4 1/4	7 1/2														1.5
2M	18	12	1 1/2	3/8	3/8	3	2 1/4	1 1/2	6 1/2	6	5	4 1/4	7 1/2														1.5
3																											
4																											
5																											

STANDARD SIGN
R9-3B

WISCONSIN DEPT OF TRANSPORTATION

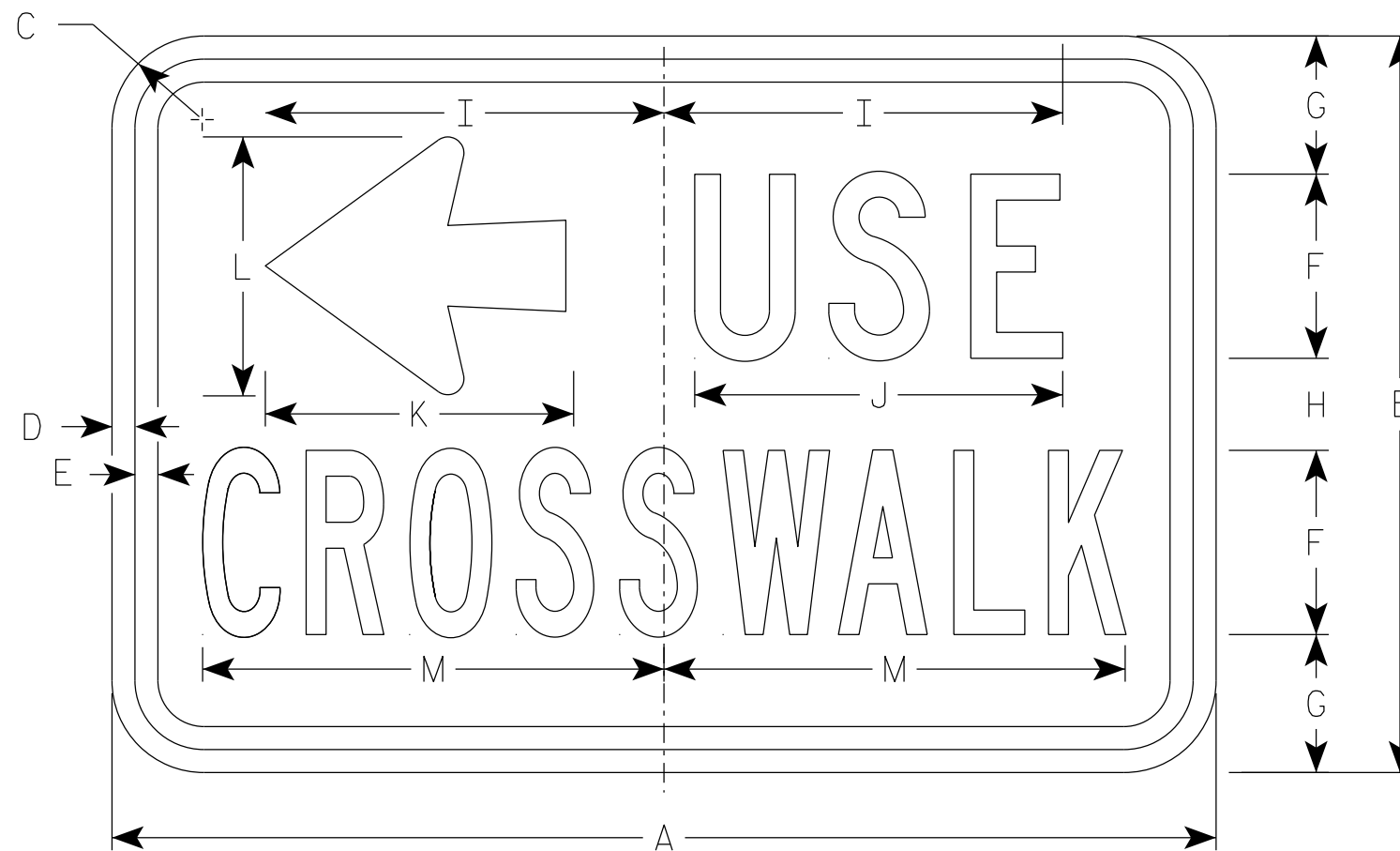
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 1/23/24 PLATE NO. R9-3B.3

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - C Line 1, Series B Line 2



R9-3BL

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	18	12	1 1/2	3/8	3/8	3	2 1/4	1 1/2	6 1/2	6	5	4 1/4	7 1/2														1.5
2M	18	12	1 1/2	3/8	3/8	3	2 1/4	1 1/2	6 1/2	6	5	4 1/4	7 1/2														1.5
3																											
4																											
5																											

STANDARD SIGN
R9-3BL

WISCONSIN DEPT OF TRANSPORTATION

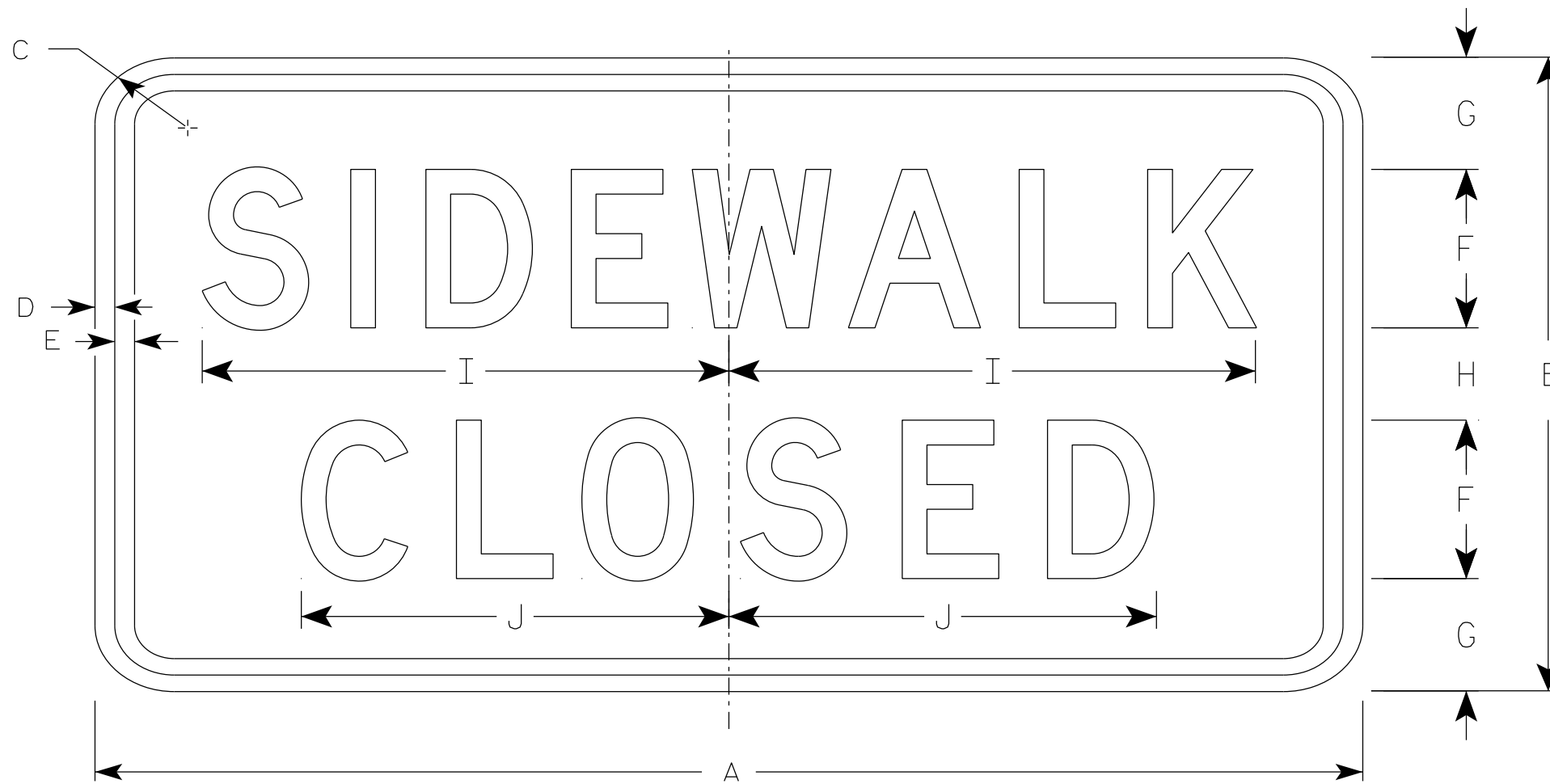
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 1/23/24 PLATE NO. R9-3BL.2

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
 - Background - White
 - Message - Black
3. Message Series - C
4. Use Size 2 for Sidewalks. Use Size 3 for Paths and Trails.



R9-9

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	12	1 1/2	1/2	1/2	3	2 1/8	1 3/4	10	8 1/8																	2.0
2M	24	12	1 1/2	1/2	1/2	3	2 1/8	1 3/4	10	8 1/8																	2.0
3	30	18	1 1/2	1/2	1/2	4	3 1/2	3	12 1/2	10 1/4																	3.75
4																											
5																											

STANDARD SIGN
R9-9

WISCONSIN DEPT OF TRANSPORTATION

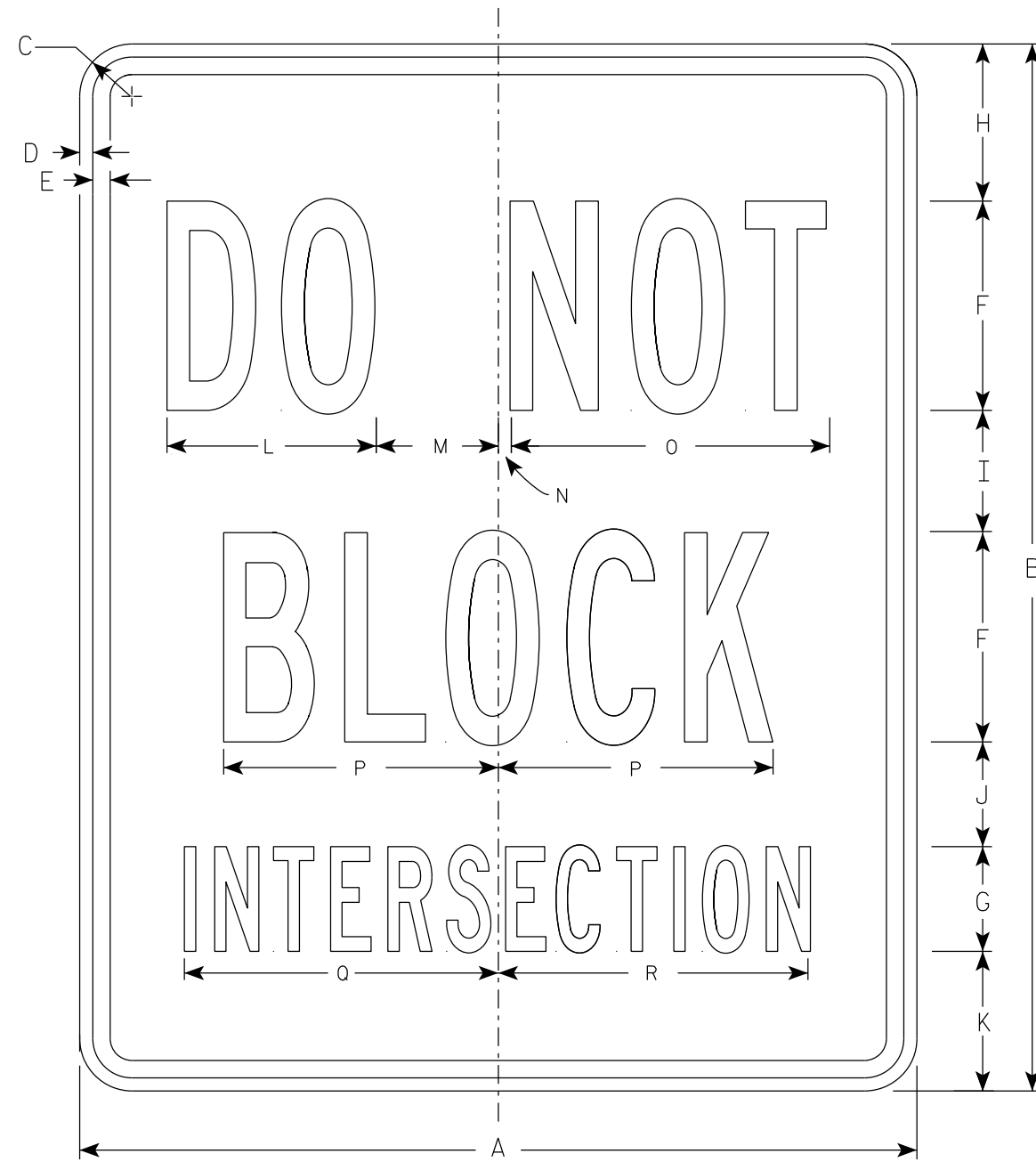
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 1/24/24 PLATE NO. R9-9.7

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
 - Background - White
 - Message - Black
3. Message Series - B



R10-7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	30	1 1/2	3/8	1/2	6	3	4 1/2	3 1/2	3	4	6	3 1/2	3/8	9 1/8	7 7/8	9	8 7/8									5.0
2M	24	30	1 1/2	3/8	1/2	6	3	4 1/2	3 1/2	3	4	6	3 1/2	3/8	9 1/8	7 7/8	9	8 7/8									5.0
3	36	48	1 7/8	1/2	5/8	10	5	7	5 1/4	4 1/2	6 1/4	10	5 5/8	1/2	15 1/8	13 1/8	15	14 7/8									12.0
4																											
5																											

STANDARD SIGN
R10-7

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 1/25/24 PLATE NO. R10-7.6

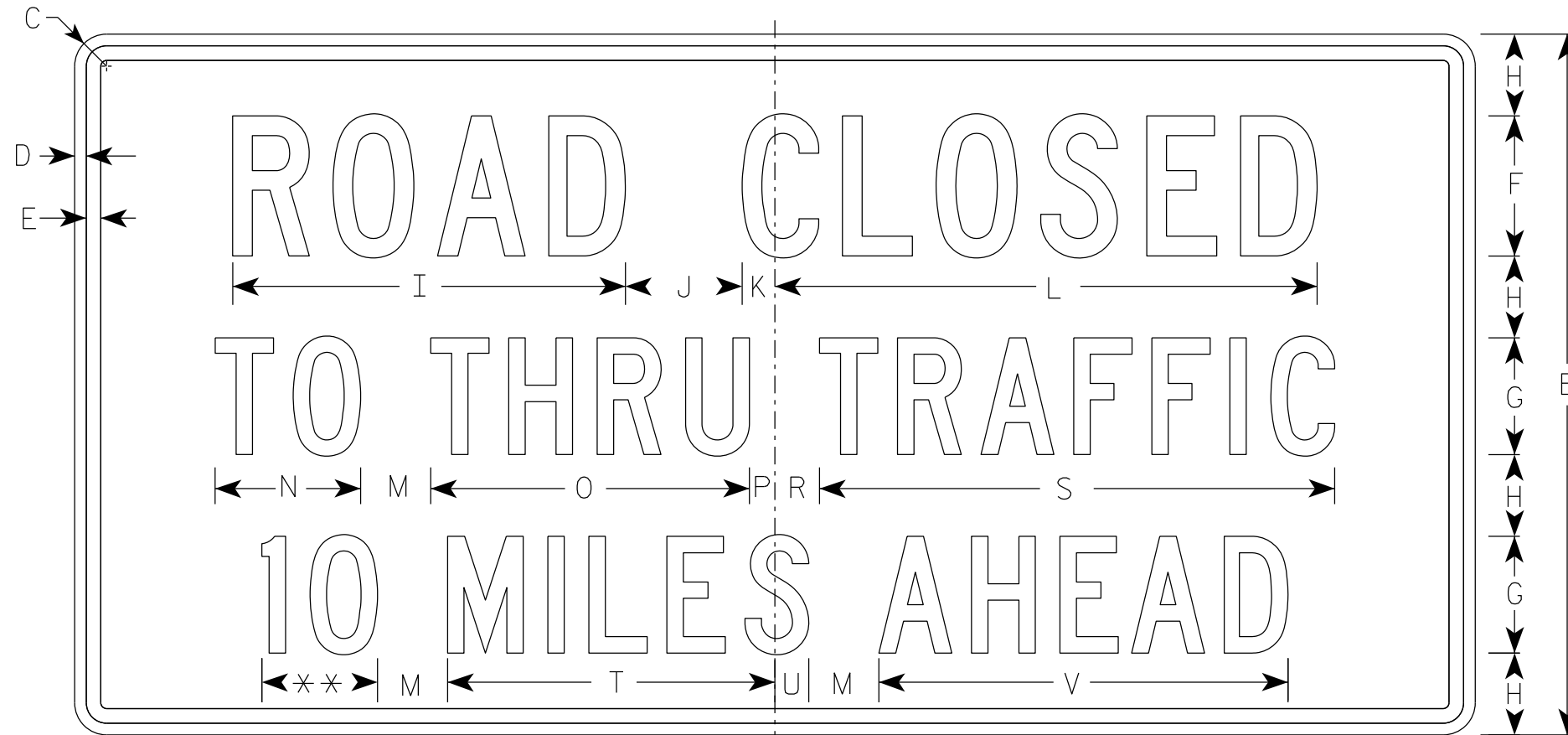
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

7

7

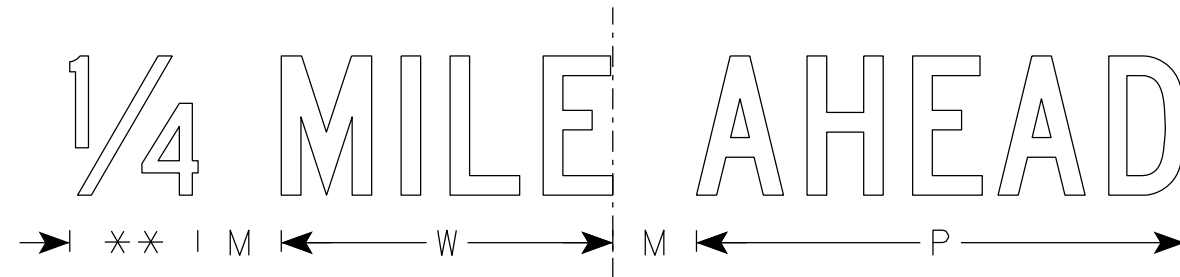
NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals to nearest quarter mile and optically adjust spacing to achieve proper balance.



R11-3

** See Note 5



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.	
1	36	18	1 1/2	3/8	3/8	4	3	2	11 1/4	3	1 1/8	15 3/8	2	3 3/4	8 1/4	5/8		1 3/8	13 1/4	8 3/8	7/8	10 1/2	7 1/8				4.5	
2S	60	30	1 7/8	1/2	5/8	6	5	3 1/2	16 7/8	5	1 3/8	23 1/4	3	6 1/4	13 5/8	1 1/8		1 7/8	22 1/8	14	1 1/2	17 1/2	11 7/8				12.5	
2M	60	30	1 7/8	1/2	5/8	6	5	3 1/2	16 7/8	5	1 3/8	23 1/4	3	6 1/4	13 5/8	1 1/8		1 7/8	22 1/8	14	1 1/2	17 1/2	11 7/8				12.5	
3																												
4																												
5																												

STANDARD SIGN
R11-3

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 2/5/24 PLATE NO. R11-3.10

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - Lines 1, 2 & 3 are series E
Lines 4, 5, & 6 are series D.
4. Substitute appropriate numeral and optically adjust spacing to achieve proper balance.
5. Substitute name of county or town on County Trunk and Town Highways respectively. Community name on City or Village Streets including Connecting Highways is optional.



* Varies (see note 4)

R12-1

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24	30	1 1/2	3/8	1/2	3	6	4	3	1 1/4	2 1/4	1 3/8	3/4	1/2	1 7/8	9	9 1/2	6	6 1/2	7 1/8	6 7/8	3 1/4	3 5/8	7 3/4		5.0	
2S	24	30	1 1/2	3/8	1/2	3	6	4	3	1 1/4	2 1/4	1 3/8	3/4	1/2	1 7/8	9	9 1/2	6	6 1/2	7 1/8	6 7/8	3 1/4	3 5/8	7 3/4		5.0	
2M	24	30	1 1/2	3/8	1/2	3	6	4	3	1 1/4	2 1/4	1 3/8	3/4	1/2	1 7/8	9	9 1/2	6	6 1/2	7 1/8	6 7/8	3 1/4	3 5/8	7 3/4		5.0	
3	36	48	1 7/8	1/2	5/8	6	10	8	4 1/2	2 1/2	2 1/4	1 1/2	3/4	1/2	3	13 1/2	14 1/4	9	9 3/4	10 5/8	10 1/4	3 1/4	3 5/8	7 3/4		12.0	
4	48	60	3	3/4	1	6	12	8	6	2 1/2	4 1/2	2 3/4	1 1/2	1	3 3/4	18	19	12	13	14 1/4	13 3/4	6 1/2	7 1/4	15 1/2		20.0	
5																											

STANDARD SIGN
R12-1

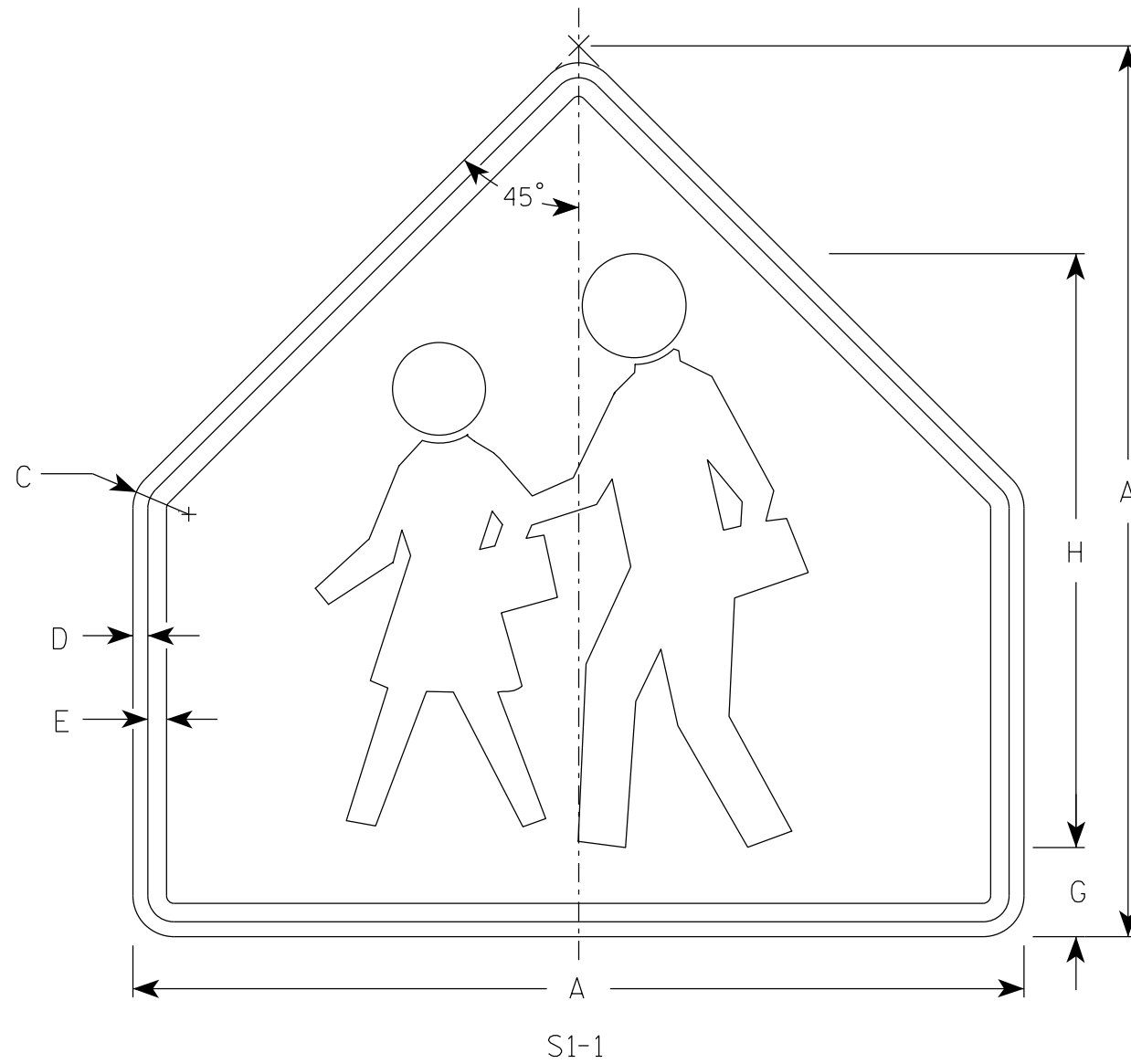
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 2/8/24 PLATE NO. R12-1.9

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
 Background - Yellow-Green
 Message - Black



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30		1 3/8	1/2	5/8		3	20																			4.69
2S	36		1 5/8	5/8	3/4		3 1/2	24																			6.75
2M	36		1 5/8	5/8	3/4		3 1/2	24																			6.75
3	36		1 5/8	5/8	3/4		3 1/2	24																			6.75
4	48		1 7/8	3/4	1		4 3/4	32																			12
5																											

STANDARD SIGN
S1-1

WISCONSIN DEPT OF TRANSPORTATION

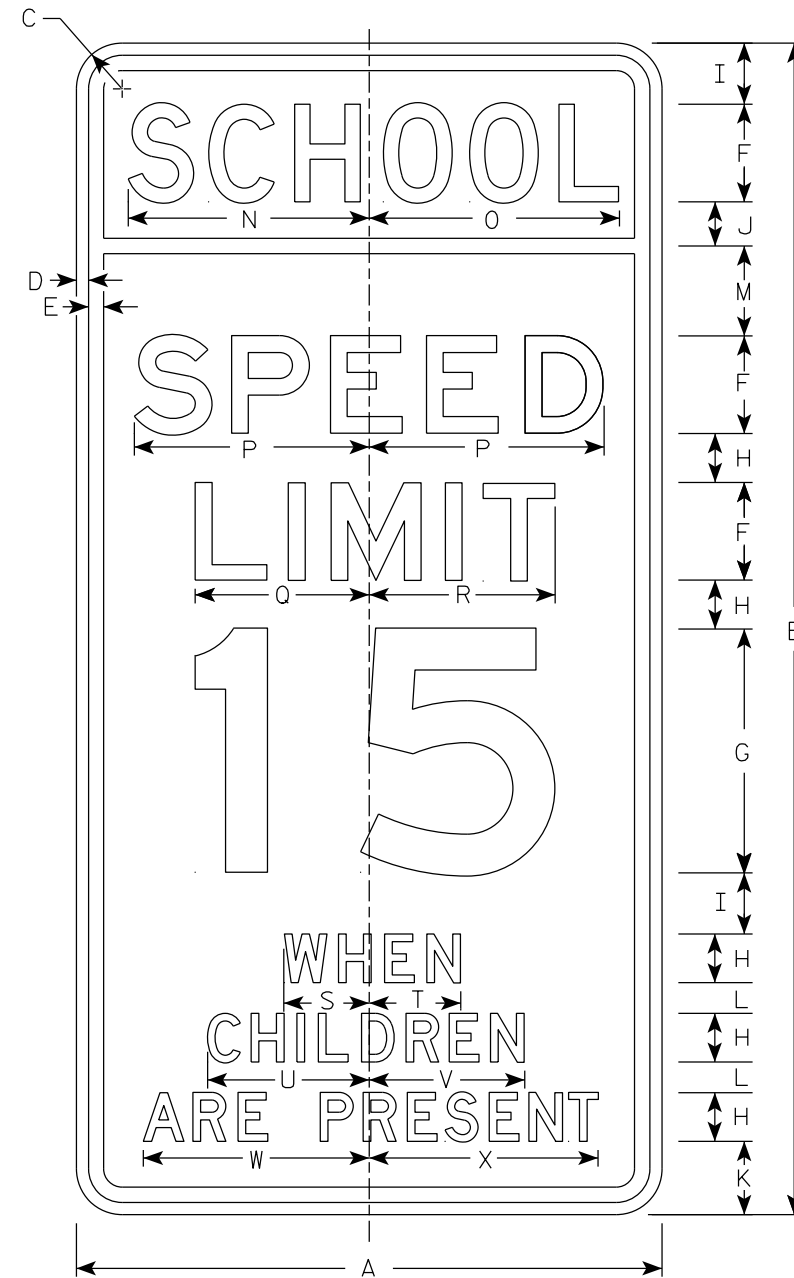
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 2/16/2023 PLATE NO. S1-1.9

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ **E**

NOTES

1. Sign is Type II - See Note 2 for Sheeting Type
2. Color:
 - Background - See Note 4
 - Message - Black
3. Message Series - See Note 5
4. Top panel (SCHOOL) background - Yellow Green - Type F Reflective
Lower panel background - White - Type SH Reflective
5. From top to bottom:
 - Lines 1, 5, 6 & 7 are series D
 - Lines 2, 3 & 4 are series E
6. Line 4 substitute appropriate numerals and adjust spacing to achieve proper balance.



S4-51

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	48	1 7/8	1/2	5/8	4	10	2	2 1/2	1 3/4	3	1 1/4	3 3/4	9 7/8	10 1/4	9 5/8	7 1/8	7 5/8	3 1/2	3 3/8	6 5/8	6 3/8	9 1/4	9 3/8		8.00	
2M	24	48	1 7/8	1/2	5/8	4	10	2	2 1/2	1 3/4	3	1 1/4	3 3/4	9 7/8	10 1/4	9 5/8	7 1/8	7 5/8	3 1/2	3 3/8	6 5/8	6 3/8	9 1/4	9 3/8		8.00	
3	36	72	3	3/4	1	6	15	3	3 3/4	2 3/4	4 1/2	1 7/8	5 1/2	15	15 1/4	14 1/2	11 1/4	11 1/2	5 1/2	5 3/4	10	9 3/4	14	14 1/8		18.00	
4																											
5																											

STANDARD SIGN
S4-51

WISCONSIN DEPT OF TRANSPORTATION

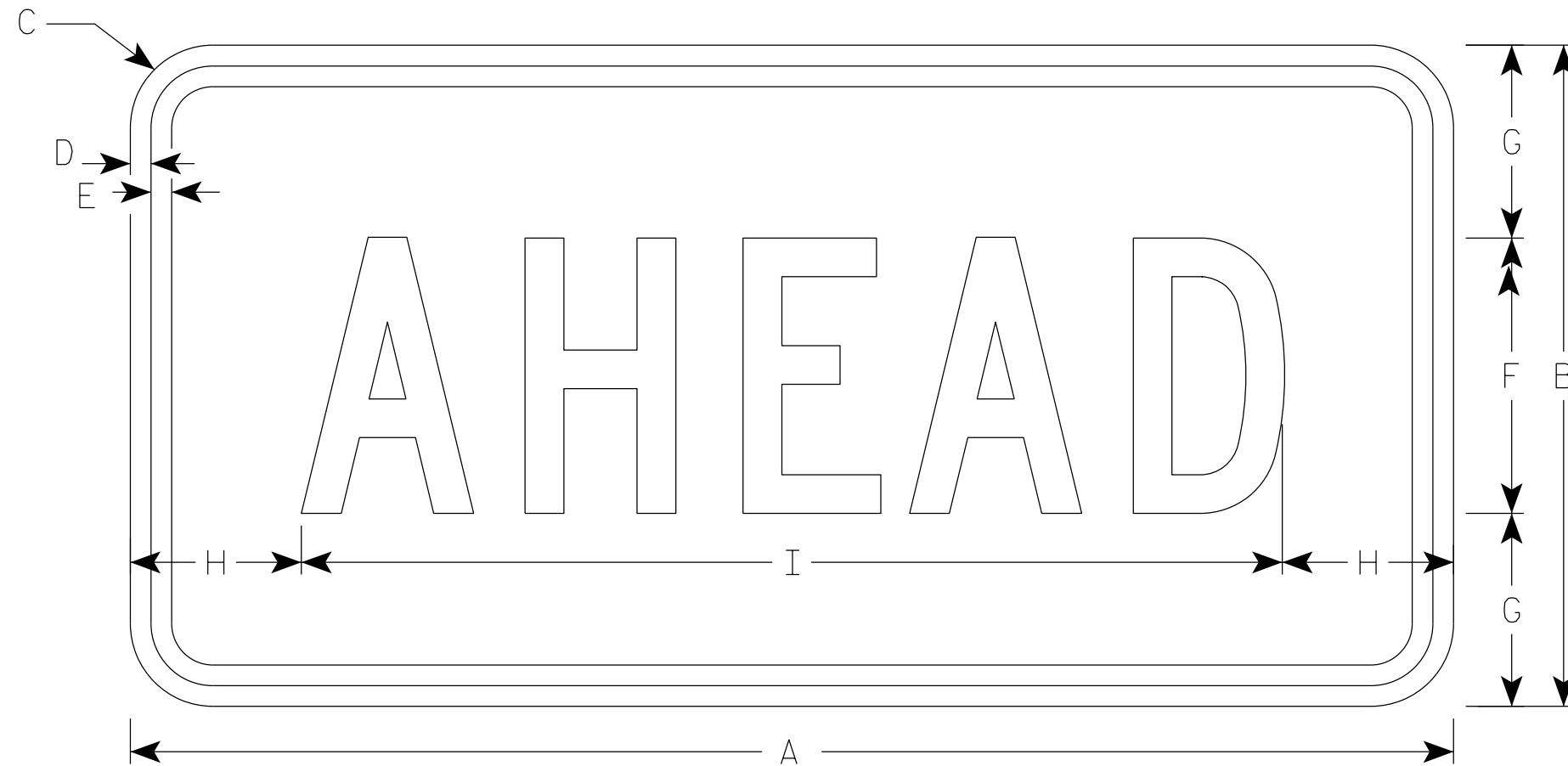
APPROVED *Matthew R Rauch*
For State Traffic Engineer

DATE 2/22/2023 PLATE NO. S4-51.11

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ **E**

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
 Background - Yellow-Green
 Message - Black
3. Message Series - C



S16-9P

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24	12	1 1/2	3/8	3/8	5	3 1/2	3 1/8	17 3/4																		2.0
2S	30	18	1 1/2	3/8	1/2	7	5 1/2	2 3/4	24 1/2																		3.75
2M	30	18	1 1/2	3/8	1/2	7	5 1/2	2 3/4	24 1/2																		3.75
3	30	18	1 1/2	3/8	1/2	7	3 1/2	2 3/4	24 1/2																		3.75
4	48	24	1 7/8	1/2	5/8	10	7	6 1/8	35 3/4																		8.0
5																											

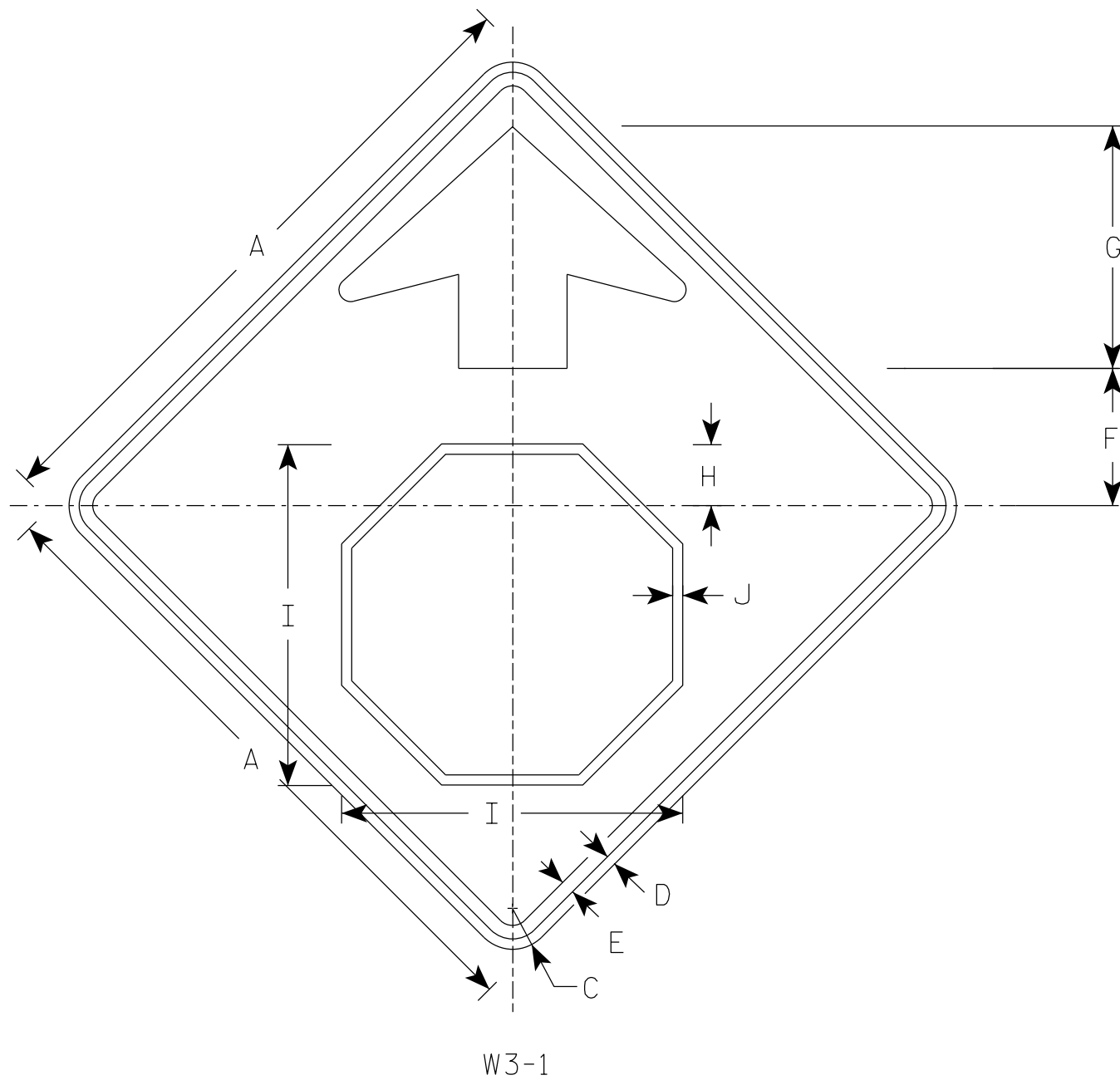
STANDARD SIGN
S16-9P

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

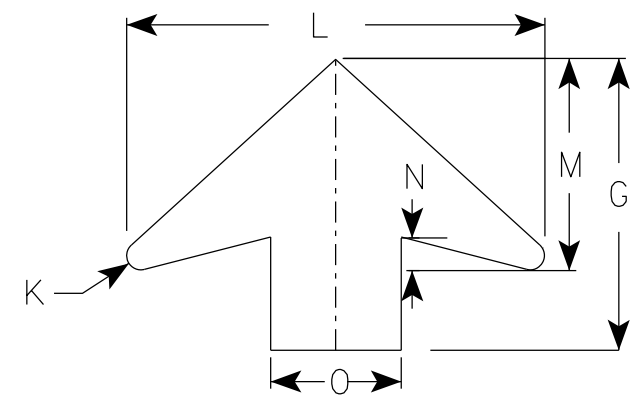
DATE 2/23/2023 PLATE NO. S16-9P.2

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**



NOTES

1. Sign is Type II - Type F Reflective
2. Color:
 - Background - Yellow
 - Arrow & Border - Black
 - Stop Symbol - White Border on Red Background



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30		1 7/8	1/2	5/8	6 1/4	11 1/4	2 7/8	15 3/4	1/2	1/2	16	8	1 1/4	5												6.25
2S	36		2 1/4	5/8	3/4	7 1/2	13 1/2	3 1/2	19	5/8	5/8	19 1/4	9 3/4	1 5/8	6												9.0
2M	36		2 1/4	5/8	3/4	7 1/2	13 1/2	3 1/2	19	5/8	5/8	19 1/4	9 3/4	1 5/8	6												9.0
3	36		2 1/4	5/8	3/4	7 1/2	13 1/2	3 1/2	19	5/8	5/8	19 1/4	9 3/4	1 5/8	6												9.0
4	48		3	3/4	1	10	17 7/8	4 1/2	25 1/8	3/4	7/8	25 5/8	13	2	8												16.0
5	48		3	3/4	1	10	17 7/8	4 1/2	25 1/8	3/4	7/8	25 5/8	13	2	8												16.0

STANDARD SIGN
W3-1

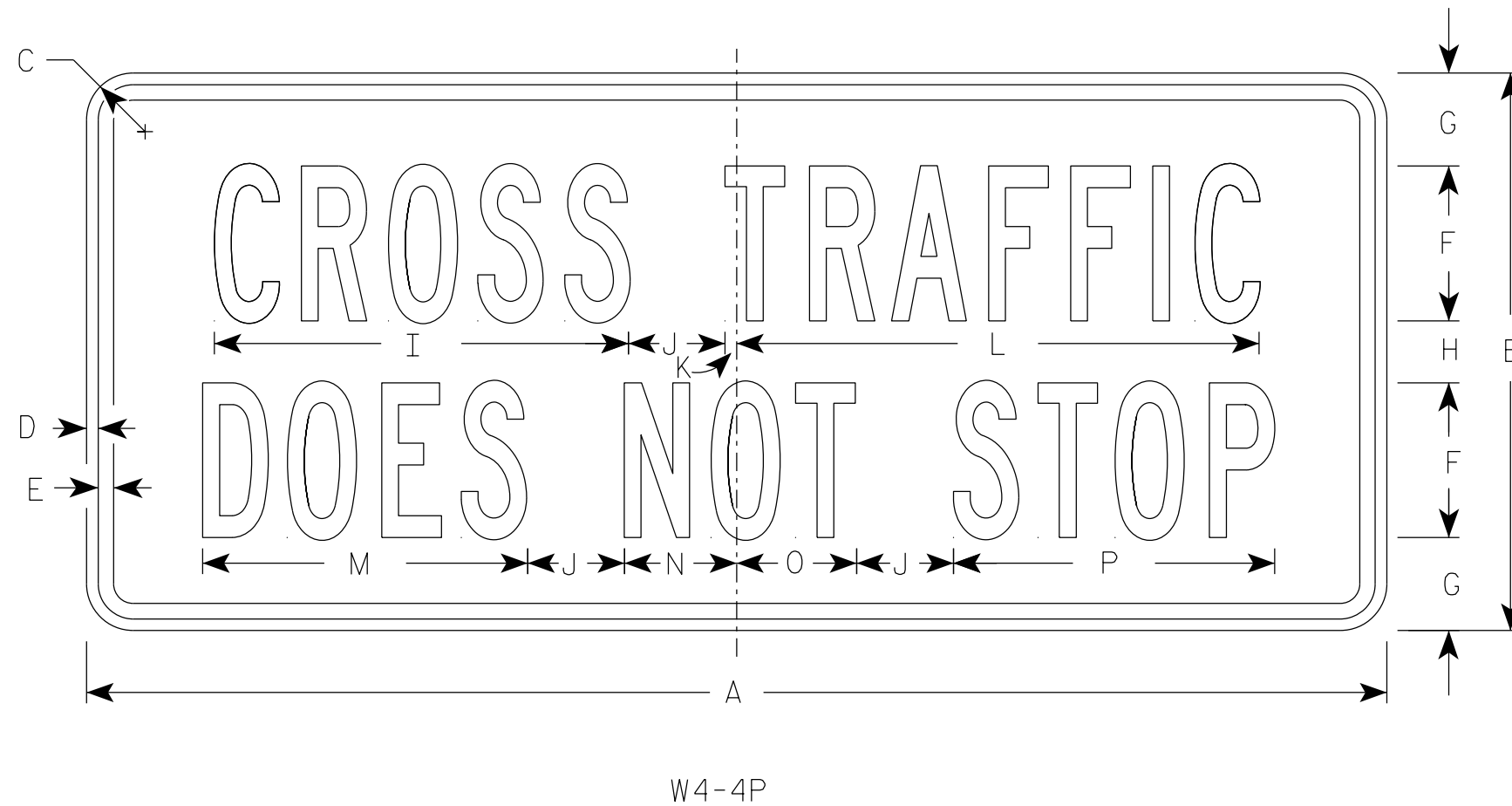
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 8/17/2023 PLATE NO. W3-1.13

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Yellow
Message - Black
3. Message Series - B



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	12	1 1/2	3/8	3/8	3	2 1/4	1 1/2	8	1 1/2	3/8	10	6 1/4	2 1/4	2 1/4	6 1/4											2.0
2M	24	12	1 1/2	3/8	3/8	3	2 1/4	1 1/2	8	1 1/2	3/8	10	6 1/4	2 1/4	2 1/4	6 1/4											2.0
3	36	15	1 1/2	3/8	1/2	4	2 5/8	1 3/4	10 3/4	2 3/8	3/8	13 1/2	8 3/8	3	3 1/8	8 3/8											3.75
4	42	18	1 1/2	3/8	1/2	5	3	2	13 3/8	3 1/8	3/8	16 7/8	10 1/2	3 5/8	3 7/8	10 3/8											5.25
5																											

STANDARD SIGN
W4-4P

WISCONSIN DEPT OF TRANSPORTATION

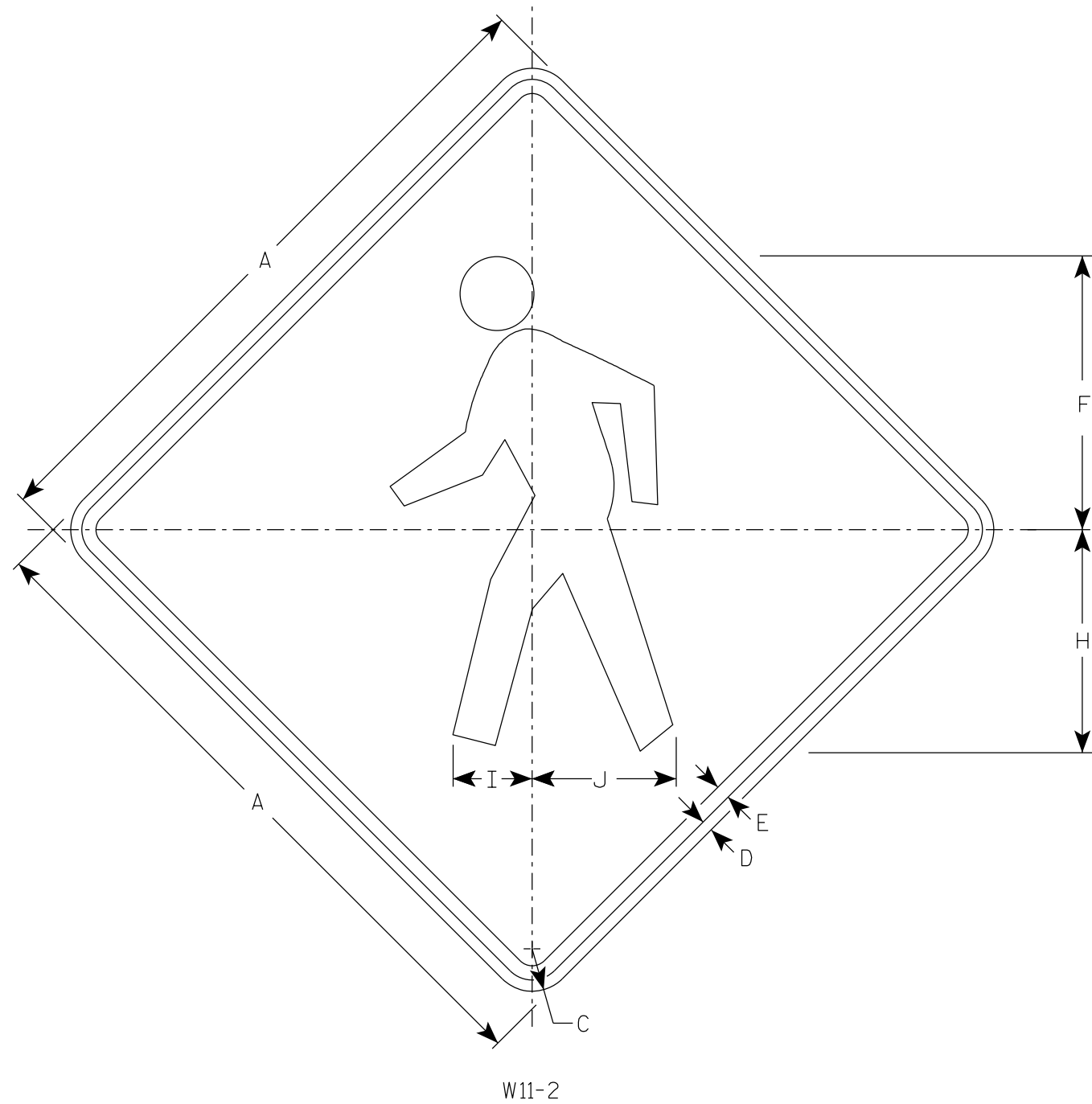
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 9/5/2023 PLATE NO. W4-4P.3

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ **E**

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
 Background - Yellow
 Message - Black



W11-2

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/2	3/8	1/2	9 3/4		7 7/8	2 7/8	5 1/8																	4.0
2S	30		1 7/8	1/2	5/8	12 1/8		9 7/8	3 1/2	6 3/8																	6.25
2M	36		2 1/4	5/8	3/4	14 1/2		11 7/8	4 1/4	7 5/8																	9.0
3	36		2 1/4	5/8	3/4	14 1/2		11 7/8	4 1/4	7 5/8																	9.0
4	48		3	3/4	1	19 3/8		15 3/4	5 5/8	10 1/4																	16.0
5																											

STANDARD SIGN
W11-2

WISCONSIN DEPT OF TRANSPORTATION

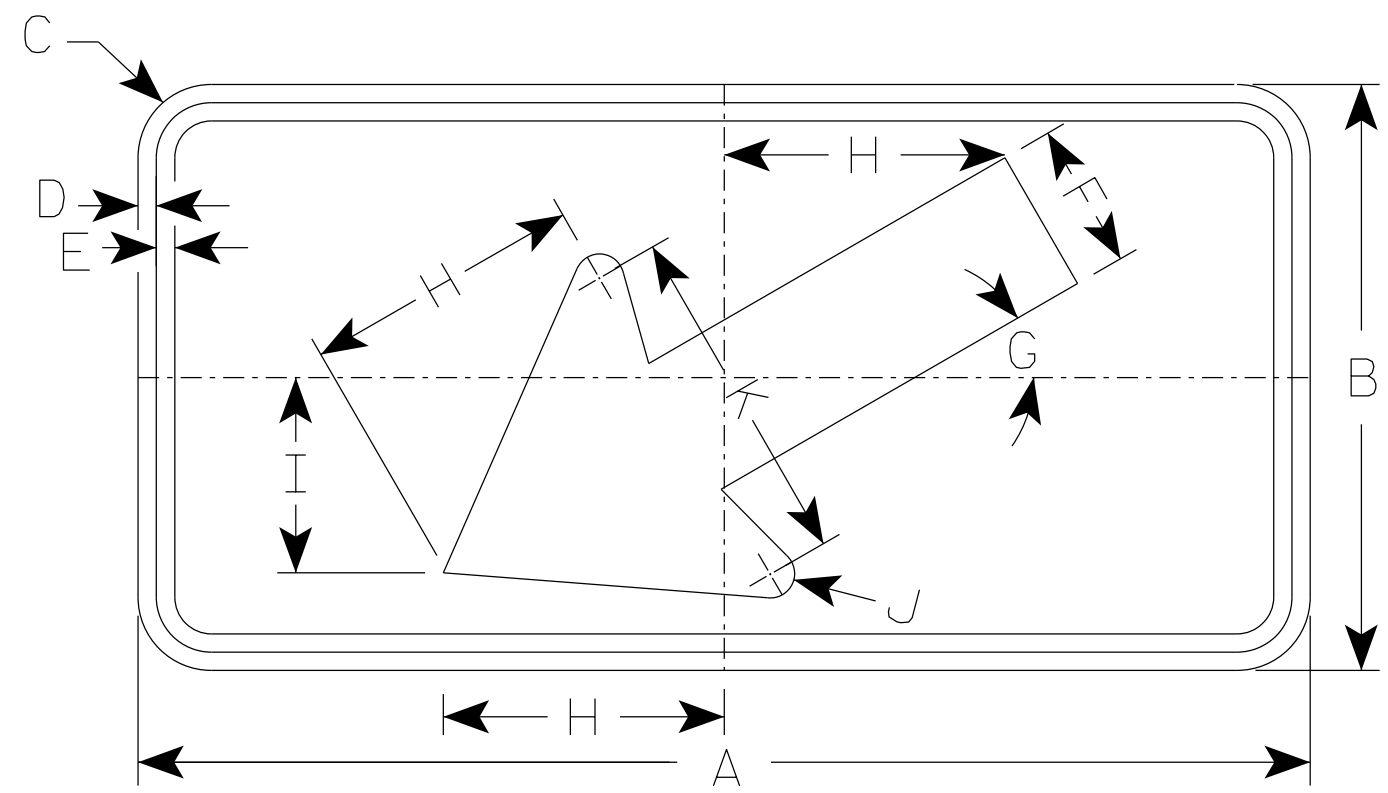
APPROVED *Matthew R. Rauch*
 For State Traffic Engineer

DATE 6/15/2023 PLATE NO. W11-2.9

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Yellow
Message - Black
3. W16-7R is the same as W16-7L
except the arrow is reversed along
the vertical centerline.



W16-7L

- * For 36" x 36" Warning Signs, use 30" x 18" W16-7L signs.
- * For 48" x 48" Warning Signs, use 48" x 24" W16-7L signs.

7

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SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	12	1 1/2	3/8	3/8	3	30°	5 3/4	4	1/2	7																2.0
* 2M	30	18	1 1/2	3/8	1/2	4 1/2	30°	8 1/2	6	5/8	10 1/4																3.75
* 3	30	18	1 1/2	3/8	1/2	4 1/2	30°	8 1/2	6	5/8	10 1/4																3.75
* 4	48	24	1 7/8	1/2	5/8	6	30°	11 1/2	8	1	14																8.0
5																											

STANDARD SIGN
W16-7

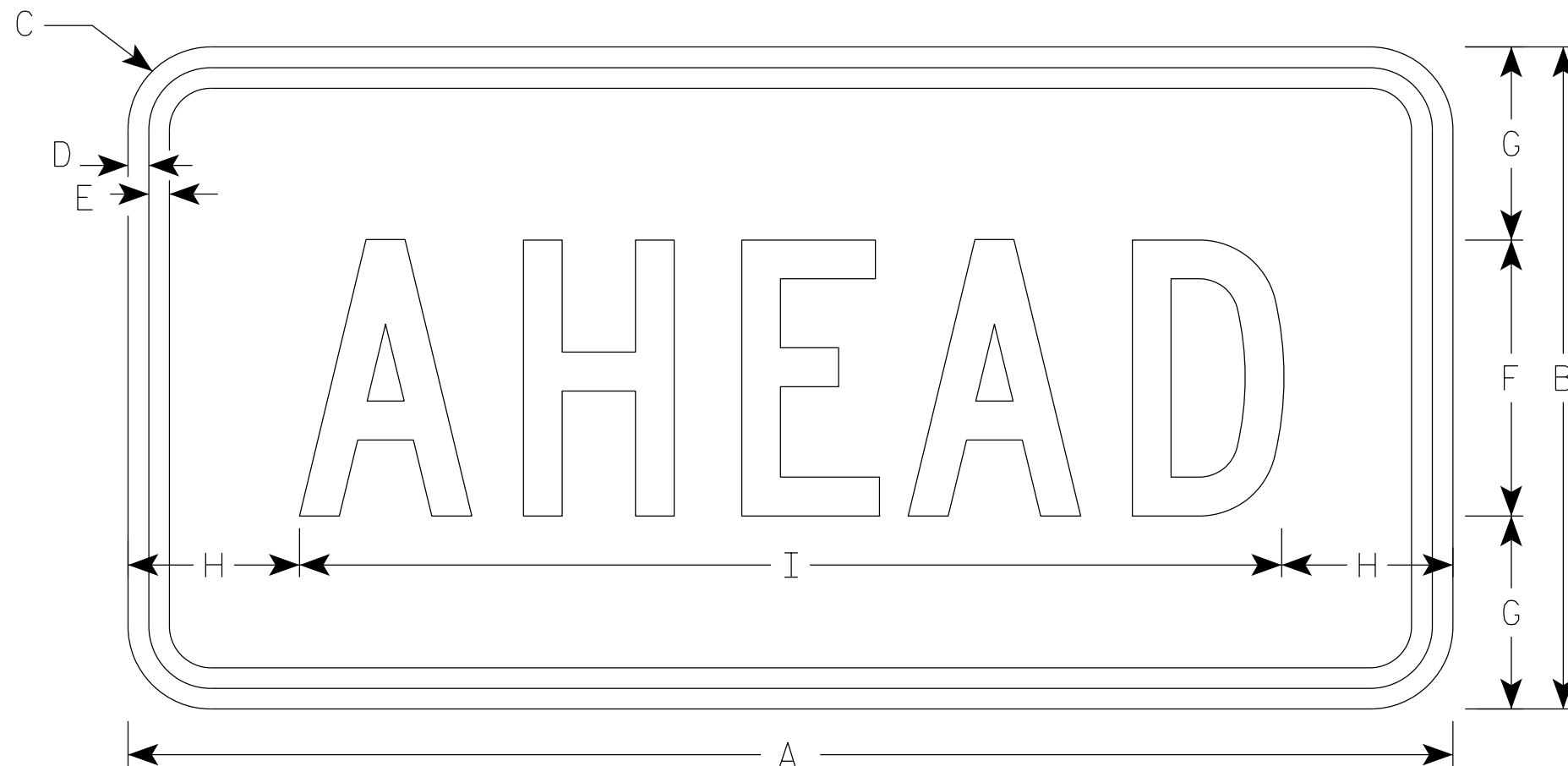
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 1/9/2024 PLATE NO. W16-7.9

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Yellow
Message - Black
3. Message Series - C



W16-9P

- * For 36" x 36" Warning Signs, use 30" x 18" W16-9P signs.
- * For 48" x 48" Warning Signs, use 48" x 24" W16-9P signs.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	12	1 1/2	3/8	3/8	5	3 1/2	3 1/8	17 3/4																		2.0
* 2M	30	18	1 1/2	3/8	1/2	7	5 1/2	2 3/4	24 1/2																		3.75
* 3	30	18	1 1/2	3/8	1/2	7	3 1/2	2 3/4	24 1/2																		3.75
* 4	48	24	1 7/8	1/2	5/8	10	7	6 1/8	35 3/4																		8.0
5																											

STANDARD SIGN
W16-9P

WISCONSIN DEPT OF TRANSPORTATION

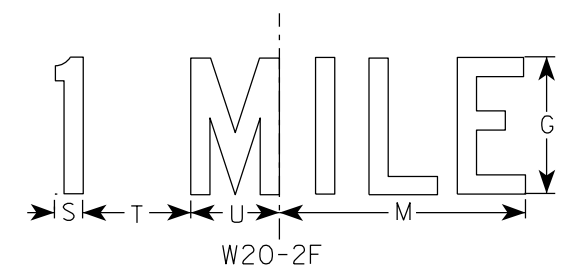
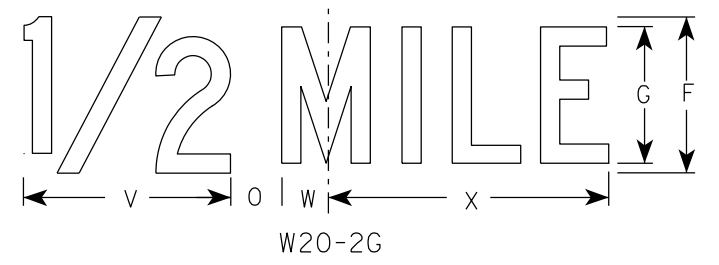
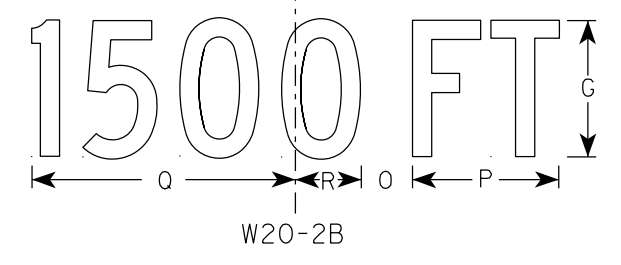
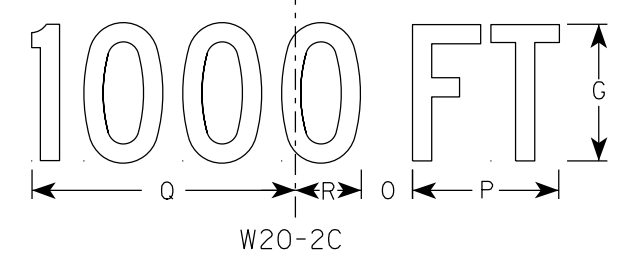
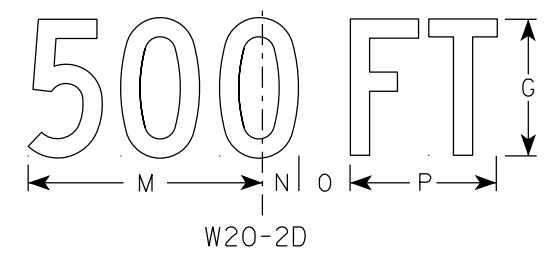
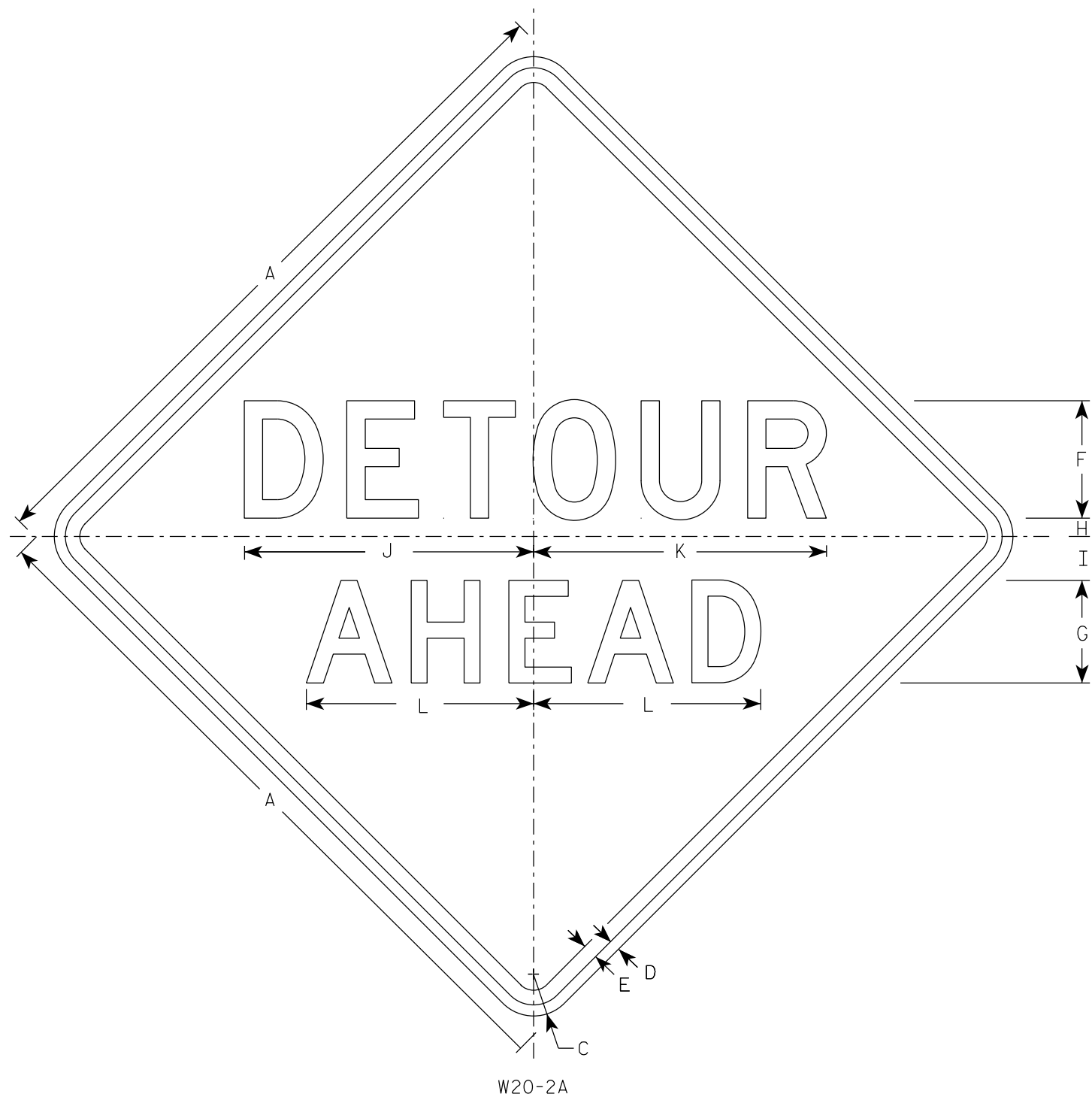
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 1/9/2024 PLATE NO. W16-9P.9

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ **E**

7

7



NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - See note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Line 1 is Series D.
Line 2 is Series D for AHEAD and Series C for all other distances.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		2 1/4	5/8	3/4	6	5	1	2 1/4	14 3/4	15	11 5/8	9	1 3/8	1 7/8	5 5/8	10 1/8	2 1/2	1 1/8	4 1/2	3 1/2	8	1 3/4	10 3/4			9.0
2S	48		3	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
2M	48		3	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
3	48		3	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
4	48		3	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
5	48		3	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0

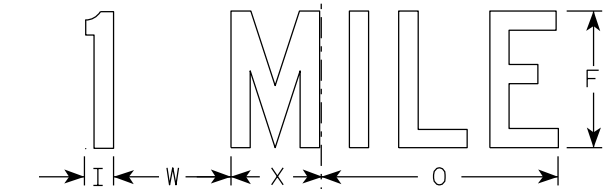
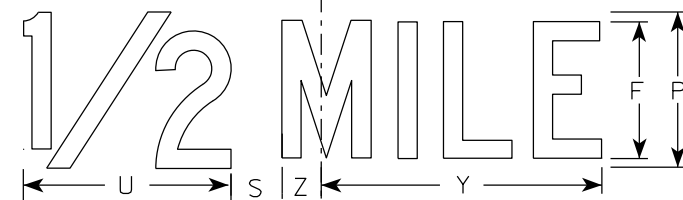
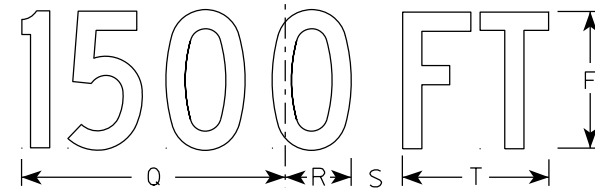
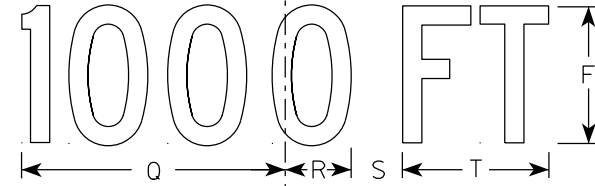
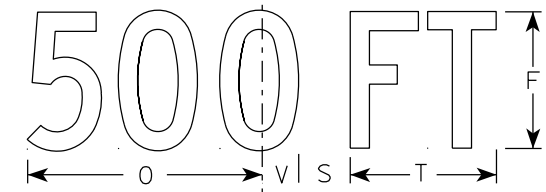
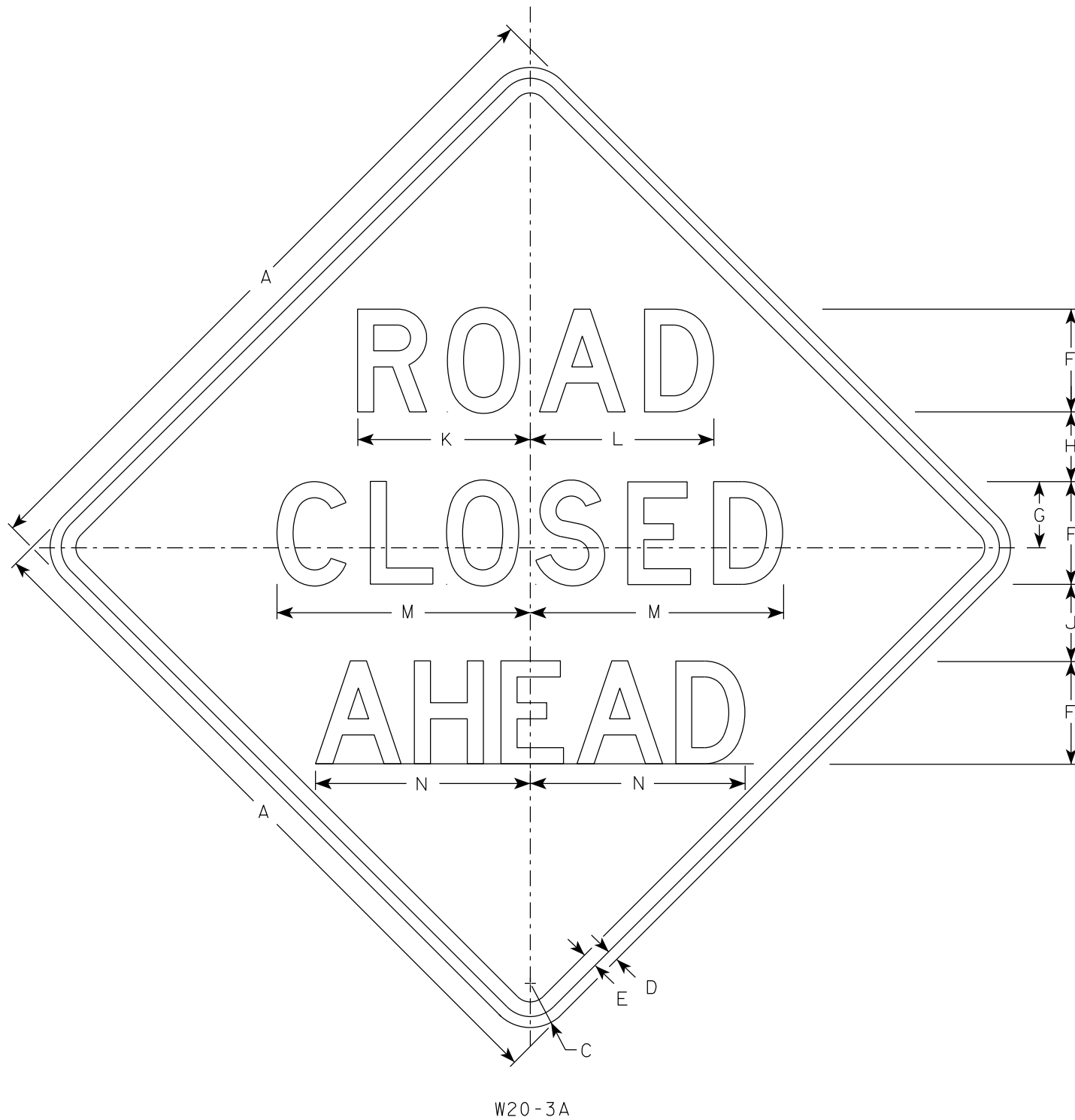
STANDARD SIGN
W20-2A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 1/10/2024 PLATE NO. W20-2.7

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ **E**



NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - see note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Lines 1 and 2 are Series D.
Line 3 is Series D for AHEAD and Series C for all other distances.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		2 1/4	5/8	3/4	5	3 3/8	3 1/2	1 1/8	4	8 3/8	8 7/8	12 1/2	11	9	6	10 1/8	2 1/2	1 7/8	5 5/8	8	1 3/8	4 1/2	3 1/2	10 3/4	1 3/4	9.0
2S	48		3	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
2M	48		3	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
3	48		3	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
4	48		3	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
5	48		3	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0

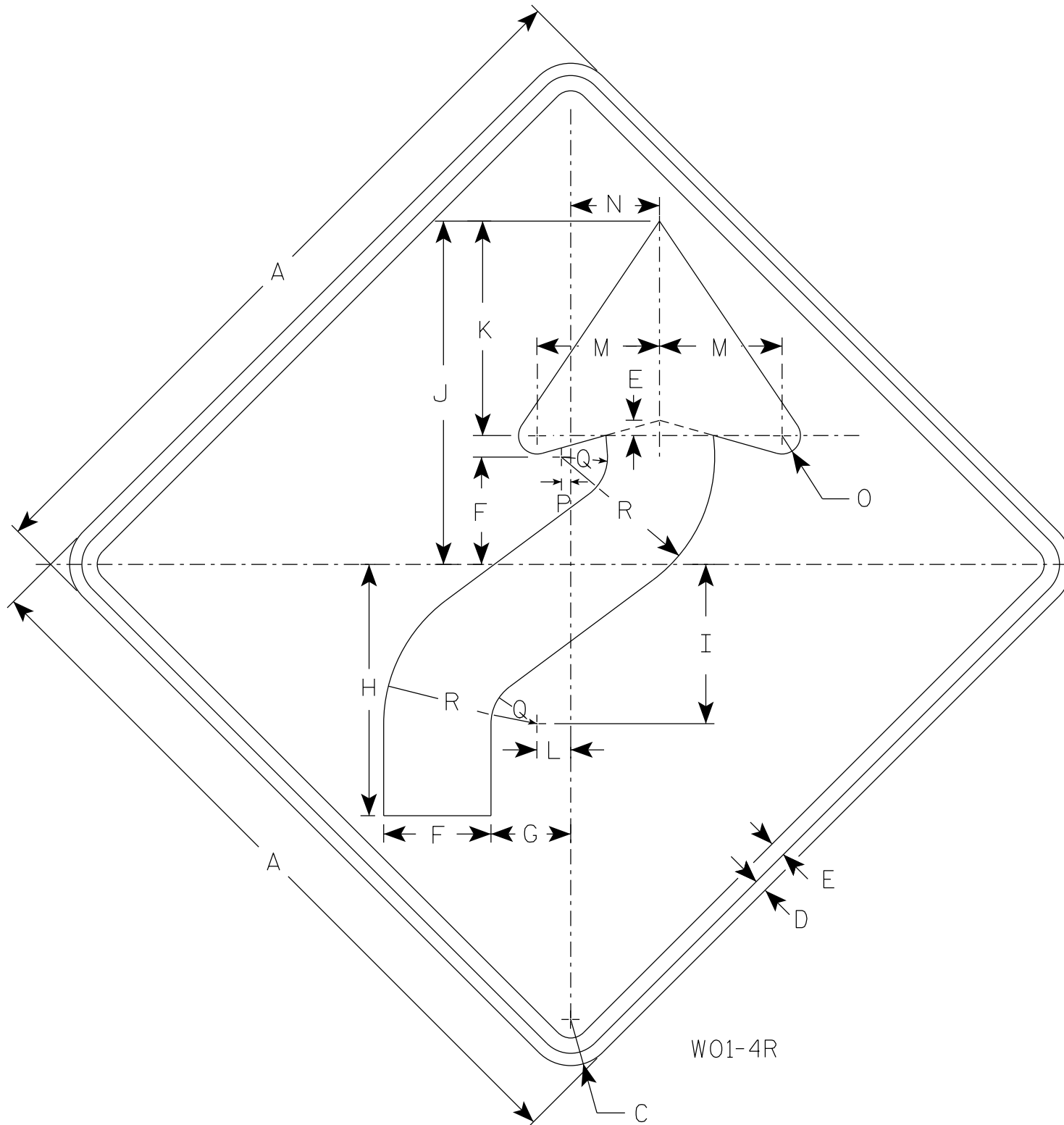
STANDARD SIGN
W20-3A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Rauch*
for State Traffic Engineer

DATE 1/10/2024 PLATE NO. W20-3.8

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**



NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. W01-4L is the same as W01-4R except the arrow is reversed along the vertical centerline.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		2 1/4	5/8	3/4	5 1/4	4	12 3/8	7 7/8	16 7/8	10 1/2	1 5/8	6	4 1/2	1	1/2	2 1/4	7 1/2									9.0
2S	48		3	3/4	1	7	5 1/4	16 1/2	10 1/2	22 1/2	14	2 1/4	8	6	1 1/4	5/8	3	10									16.0
2M	48		3	3/4	1	7	5 1/4	16 1/2	10 1/2	22 1/2	14	2 1/4	8	6	1 1/4	5/8	3	10									16.0
3	48		3	3/4	1	7	5 1/4	16 1/2	10 1/2	22 1/2	14	2 1/4	8	6	1 1/4	5/8	3	10									16.0
4	48		3	3/4	1	7	5 1/4	16 1/2	10 1/2	22 1/2	14	2 1/4	8	6	1 1/4	5/8	3	10									16.0
5	48		3	3/4	1	7	5 1/4	16 1/2	10 1/2	22 1/2	14	2 1/4	8	6	1 1/4	5/8	3	10									16.0

STANDARD SIGN
W01-4

WISCONSIN DEPT OF TRANSPORTATION

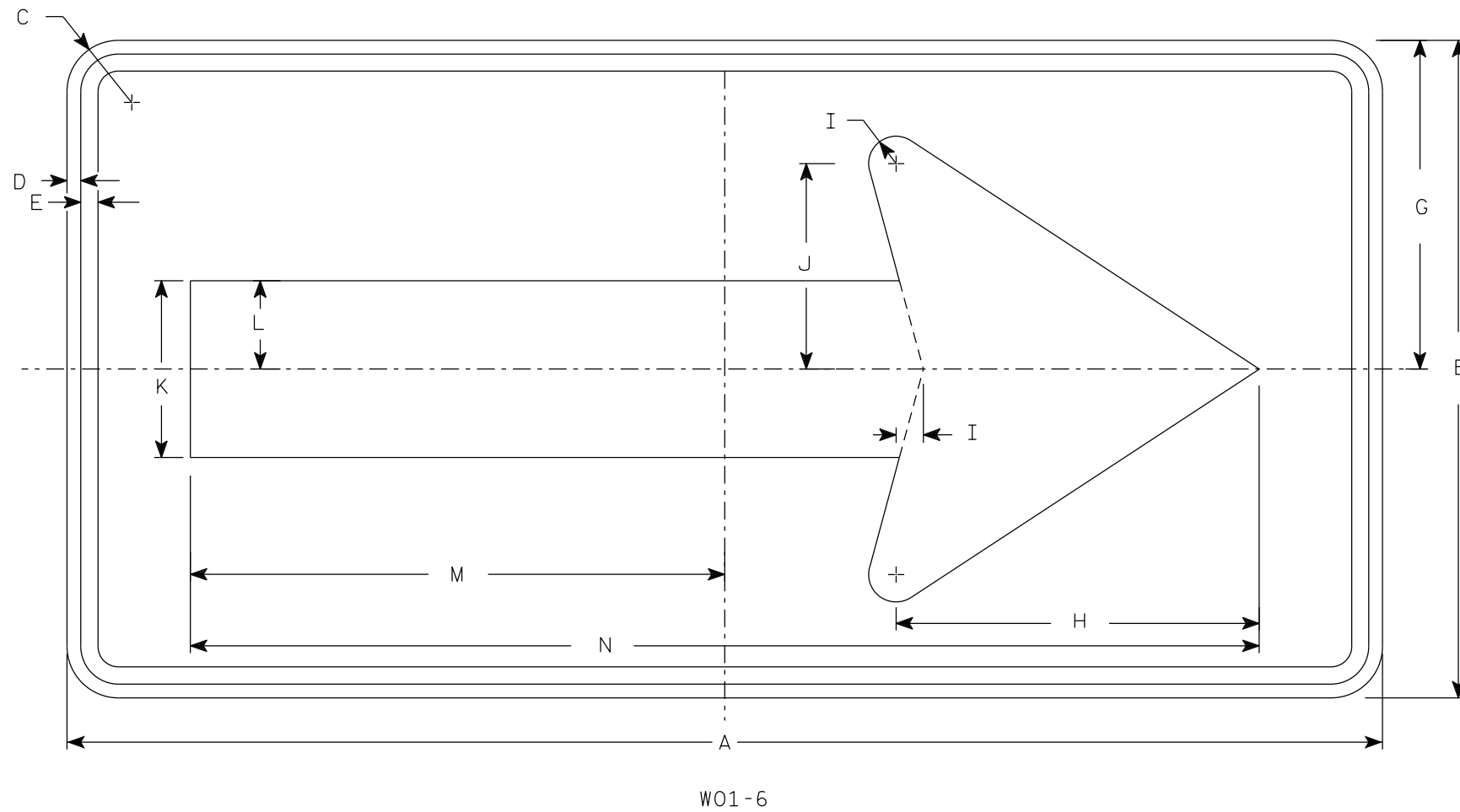
APPROVED *Matthew R Rauch*
for State Traffic Engineer

DATE 1/24/2024 PLATE NO. W01-4.2

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ **E**

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48	24	1 7/8	1/2	5/8		12	13 1/4	1	7 1/2	6 1/2	3 1/4	19 1/2	39													8.0
2M	48	24	1 7/8	1/2	5/8		12	13 1/4	1	7 1/2	6 1/2	3 1/4	19 1/2	39													8.0
3	60	30	1 7/8	1/2	5/8		15	16 1/4	1 1/4	9 1/4	8	4	24 3/8	48 3/4													12.5
4	60	30	1 7/8	1/2	5/8		15	16 1/4	1 1/4	9 1/4	8	4	24 3/8	48 3/4													12.5
5	60	30	1 7/8	1/2	5/8		15	16 1/4	1 1/4	9 1/4	8	4	24 3/8	48 3/4													12.5

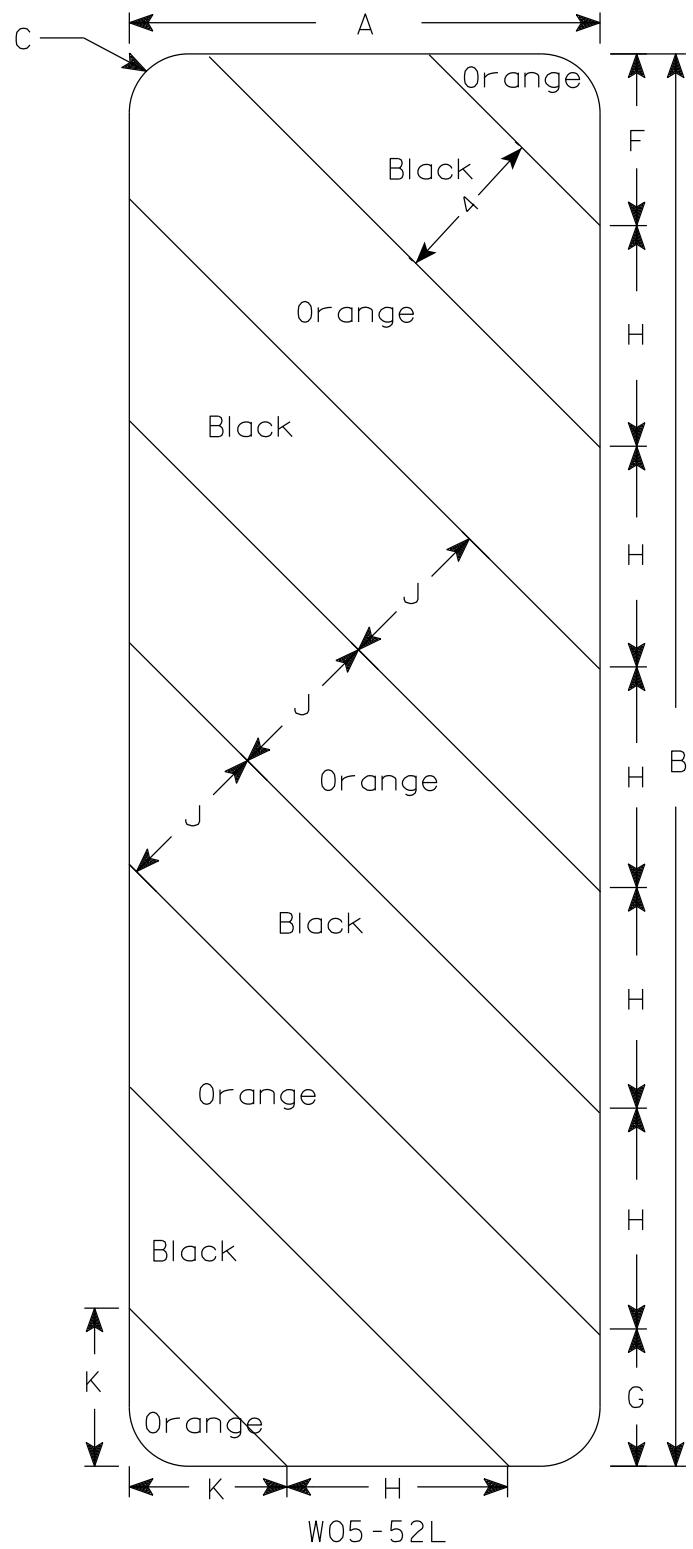
STANDARD SIGN
W01-6

WISCONSIN DEPT OF TRANSPORTATION

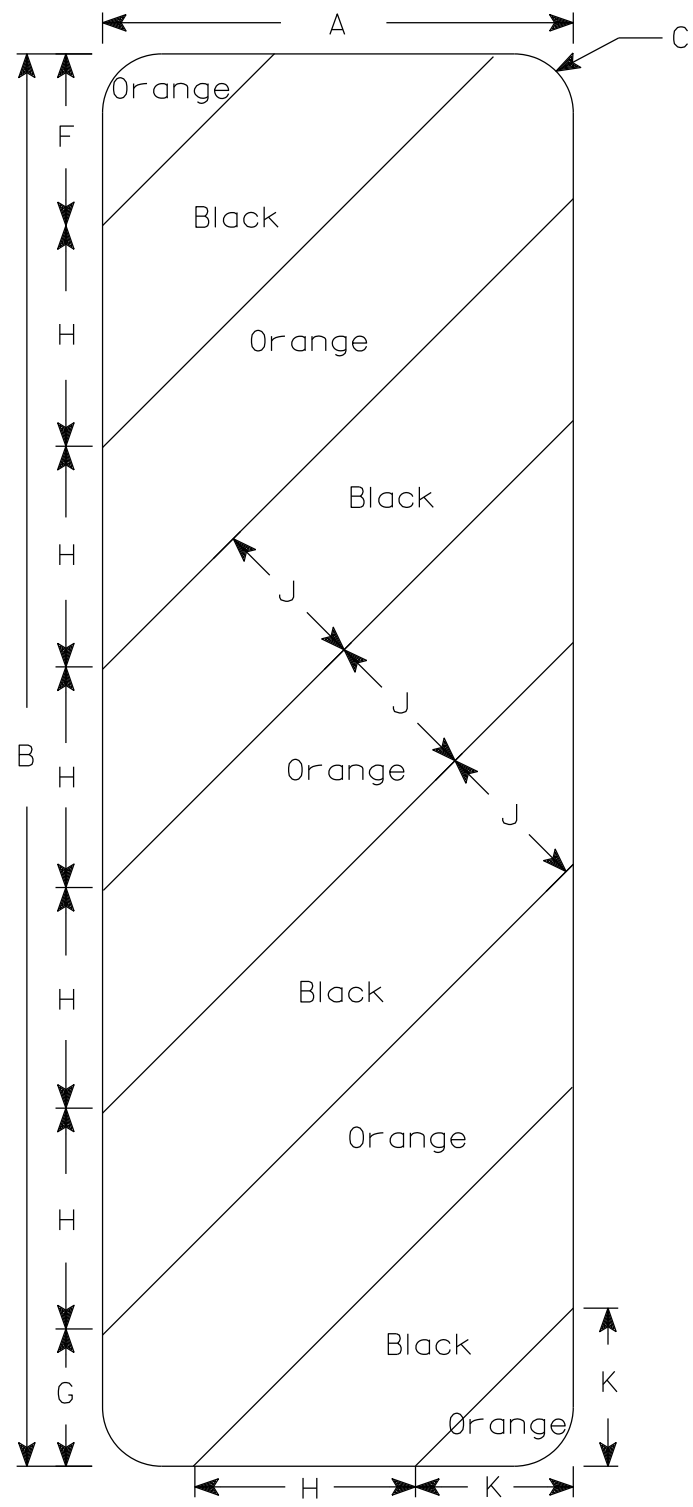
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 1/24/2024 PLATE NO. W01-6.2

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**



W05-52L



W05-52R

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
4. Alternate colors of stripes as shown.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	12	36	1 1/2			4 3/8	3 1/2	5 5/8	45°	4	4																3.0
2M	12	36	1 1/2			4 3/8	3 1/2	5 5/8	45°	4	4																3.0
3	18	54	1 1/2			6	5 1/2	8 1/2	45°	6	6 9/16																6.75
4																											
5																											

STANDARD SIGN
W05-52L & W05-52R

WISCONSIN DEPT OF TRANSPORTATION

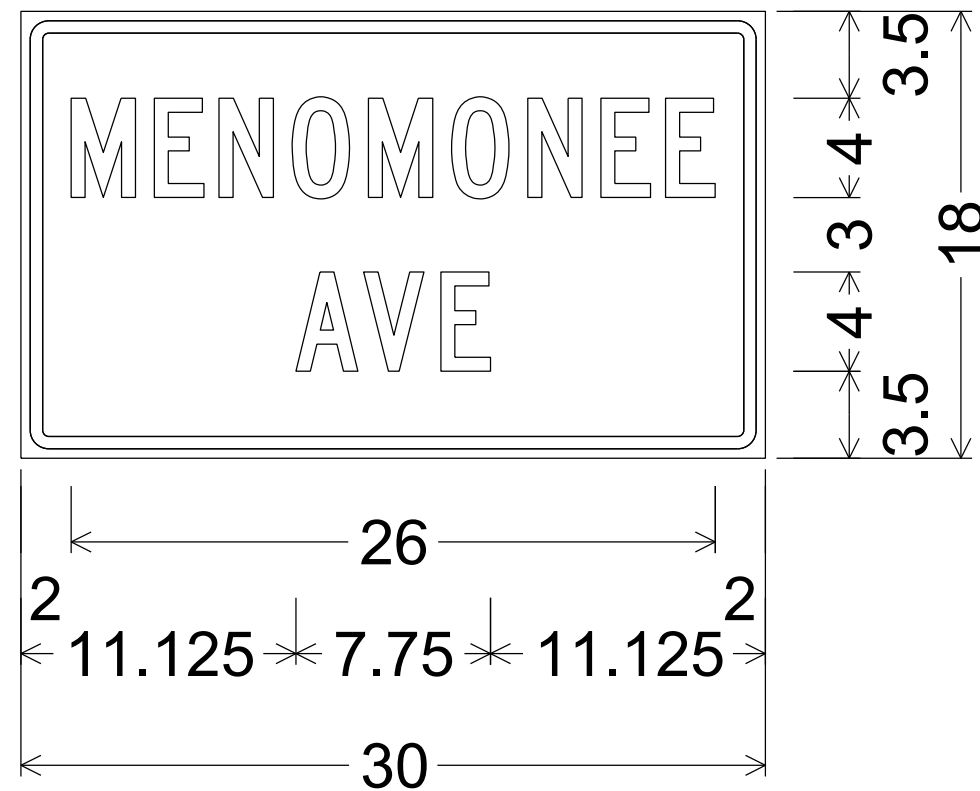
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 3/13/2024 PLATE NO. W05-52.2

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ **E**

NOTES

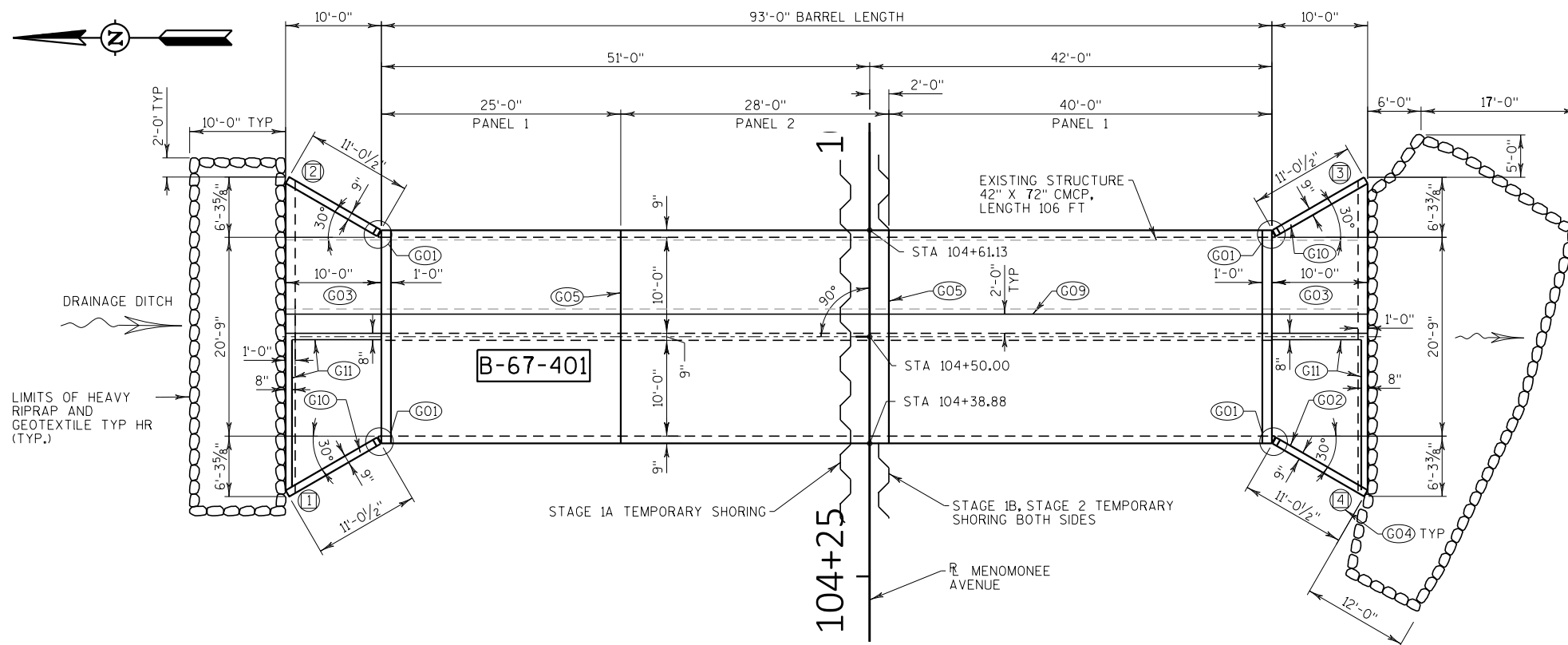
1. Fixed Message Sign Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - C



1.125" Radius, 0.500" Border, 0.375" Indent

7

7



PLAN

(TWO CELL CONCRETE BOX CULVERT)

DESIGN DATA

LIVE LOAD:

DESIGN LOADING: HL-93
 INVENTORY RATING FACTOR: RF= 1.40
 OPERATING RATING FACTOR: RF= 1.82
 WISCONSIN STANDARD PERMIT
 VEHICLE (WIS-SPV) = 295 KIPS

EARTH LOAD: DESIGNED FOR FILL HEIGHT
 RANGE OF 2.0 TO 3.0 FEET

MATERIAL PROPERTIES:

CONCRETE MASONRY $f'c = 3,500$ P.S.I.
 BAR STEEL REINFORCEMENT $f_y = 60,000$ P.S.I.

HYDRAULIC DATA

100 YEAR FREQUENCY
 $Q_{100} = 360$ C.F.S.
 VELOCITY = 4.07 F.P.S.
 $HW_{100} = EL. 844.91$
 WATERWAY AREA = 147 SQ. FT.
 DRAINAGE AREA = 169.0 ACRES
 SCOUR CRITICAL CODE = 8

2 YEAR FREQUENCY

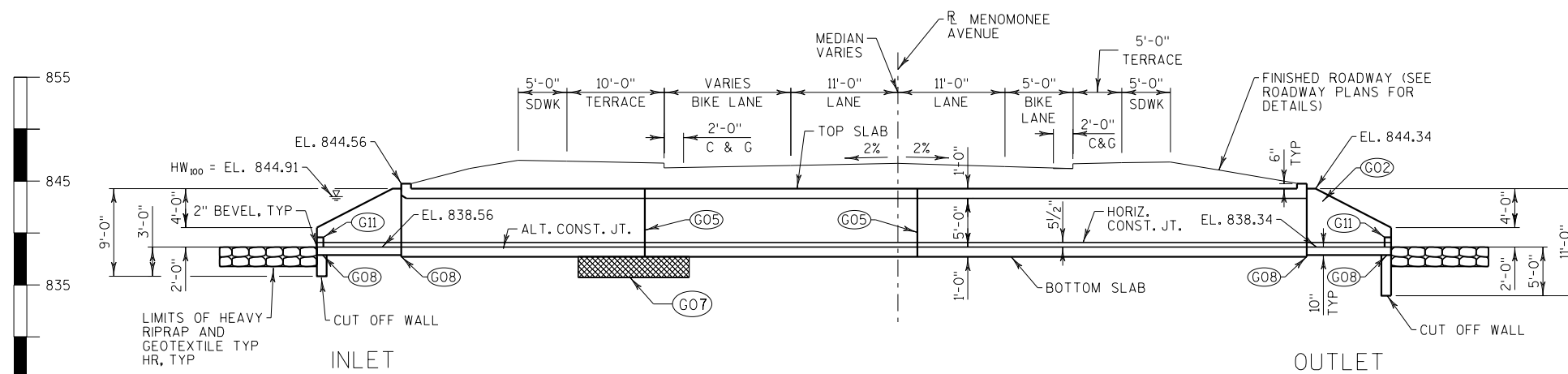
$Q_2 = 104$ C.F.S.
 VELOCITY = 1.69 F.P.S.
 $HW_2 = EL. 842.26$

LEGEND

- G01 SEE CORNER DETAILS ON SHEET 7
- G02 NAME PLATE LOCATIONS (SEE SHEET 7)
- G03 BUILD APRON AND END OF BOX LEVEL
- G04 INDICATES WING NUMBER
- G05 VERT. CONST. JOINT (TYP)
- G06 SEE ROADWAY PLANS FOR UTILITY COORDINATION
- G07 UNDER CUT 1'-0". EXCAVATION FOR UNDER CUT TO BE INCLUDED IN EXCAVATION FOR STRUCTURES. PLACE "GEOTEXTILE TYPE C" AND BACKFILL WITH "BREAKER RUN". EXTEND 3'-0" BEYOND THE FOOTPRINT OF THE CULVERT.
- G08 OPTIONAL CONSTRUCTION JOINT
- G09 LONGITUDINAL CONSTRUCTION JOINT
- G10 SEE SHEET 8 FOR RAILING PIPE AT APRON WING WALLS, TYP
- G11 WEIR WALL AT WEST CELL NORTH AND SOUTH APRONS. SEE SHEET 6 FOR DETAIL.

TRAFFIC VOLUME

MENOMONEE AVENUE
 A.D.T. = 7,900 (2025)
 A.D.T. = 8,700 (2024)
 R.D.S. = 35 M.P.H.



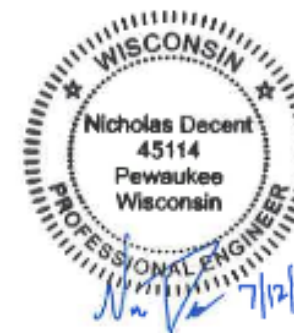
ELEVATION

BENCH MARK

NO.	STATION	ELEVATION	DESCRIPTION
1001	99+71.7, 48.3 LT	849.56	BM NAIL E FACE PP 75-7011

LIST OF DRAWINGS

1. GENERAL PLAN
2. SECTION, DETAILS & QUANTITIES
3. STAGING DETAILS
4. SUBSURFACE EXPLORATION
5. BOX DETAILS
6. APRON
7. APRON DETAILS
8. RAILING PIPE



STRUCTURE DESIGN CONTACTS:
 AARON BONK (WISDOT) (608) 261-0261
 NICK DECENT (R.A. SMITH) (262)-317-3362

NO.	DATE	REVISION	BY

raSmith
 CREATIVITY BEYOND ENGINEERING
 16745 W. Bluemound Road
 Brookfield, WI 53005-5938
 (262) 781-1000
 rasmith.com

STATE OF WISCONSIN
 DEPARTMENT OF TRANSPORTATION
 ACCEPTED *[Signature]* SDR **09/10/24**
 CHIEF STRUCTURES DESIGN ENGINEER DATE

STRUCTURE B-67-401

MENOMONEE AVENUE OVER DRAINAGE DITCH			
COUNTY	WAUKESHA	TOWN/CITY/VILLAGE	MENOMONEE FALLS
DESIGN SPEC.	AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS		
DESIGNED BY	NLD	DESIGN CK'D.	HUL
DRAWN BY	NLD	PLANS CK'D.	HUL

GENERAL PLAN AND ELEVATION
 SHEET 1 OF 8

TOTAL ESTIMATED QUANTITIES

BID ITEM NUMBER	BID ITEMS	UNIT	TOTALS
206.2001	EXCAVATION FOR STRUCTURES CULVERTS B-67-401	EACH	1
210.2500	BACKFILL STRUCTURE TYPE B	TON	1,432
311.0110	BREAKER RUN	TON	234
504.0100	CONCRETE MASONRY CULVERTS	CY	196
505.0400	BAR STEEL REINFORCEMENT HS STRUCTURES	LB	26,890
505.0600	BAR STEEL REINFORCEMENT HS COATED STRUCTURES	LB	860
511.1200	TEMPORARY SHORING B-67-401	SF	800
513.2001	RAILING PIPE	LF	44
516.0500	RUBBERIZED MEMBRANE WATERPROOFING	SY	45
606.0300	RIPRAP HEAVY	CY	81
645.0105	GEOTEXTILE TYPE C	SY	292
645.0120	GEOTEXTILE TYPE HR	SY	121
SPV.0195.01	BOX CULVERT FILL MATERIAL	TON	80
NON-BID ITEMS			
	FILLER	SIZE	¾"

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.

BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2" CLEAR UNLESS OTHERWISE SHOWN OR NOTED. THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE.

THE UPPER LIMITS OF "EXCAVATION FOR STRUCTURES BRIDGES B-67-401" SHALL BE THE EXISTING GROUNDLINE.

THE BACKFILL QUANTITIES ARE BASED ON THE PAY LIMITS SHOWN ON THE PLANS AND MAY NOT REFLECT ACTUAL PLACED QUANTITIES. "BACKFILL STRUCTURE TYPE B" REQUIRED ON THE BOX CULVERT SIDES AND BEHIND APRON WINGS FOR 3 FEET. BACKFILL PLACED BEYOND PAY LIMITS OR EXCEEDING PLAN QUANTITIES SHALL BE INCIDENTAL TO EXCAVATION FOR STRUCTURES.

IN LIEU OF USING BREAKER RUN FOR THE BOX CONSTRUCTION PLATFORM, THE CONTRACTOR MAY ELECT TO SUBSTITUTE #1 OR #2 CONCRETE COARSE AGGREGATE, SELECT CRUSHED MATERIAL OR OTHER GRANULAR MATERIAL AS APPROVED BY THE ENGINEER, THE CONTRACTOR IS RESPONSIBLE FOR BASE STABILITY WITH ANY SUBSTITUTED MATERIAL. THE REGION GEOTECHNICAL ENGINEER MAY BE CONTACTED TO DETERMINE IF "OTHER GRANULAR MATERIAL" IS ACCEPTABLE.

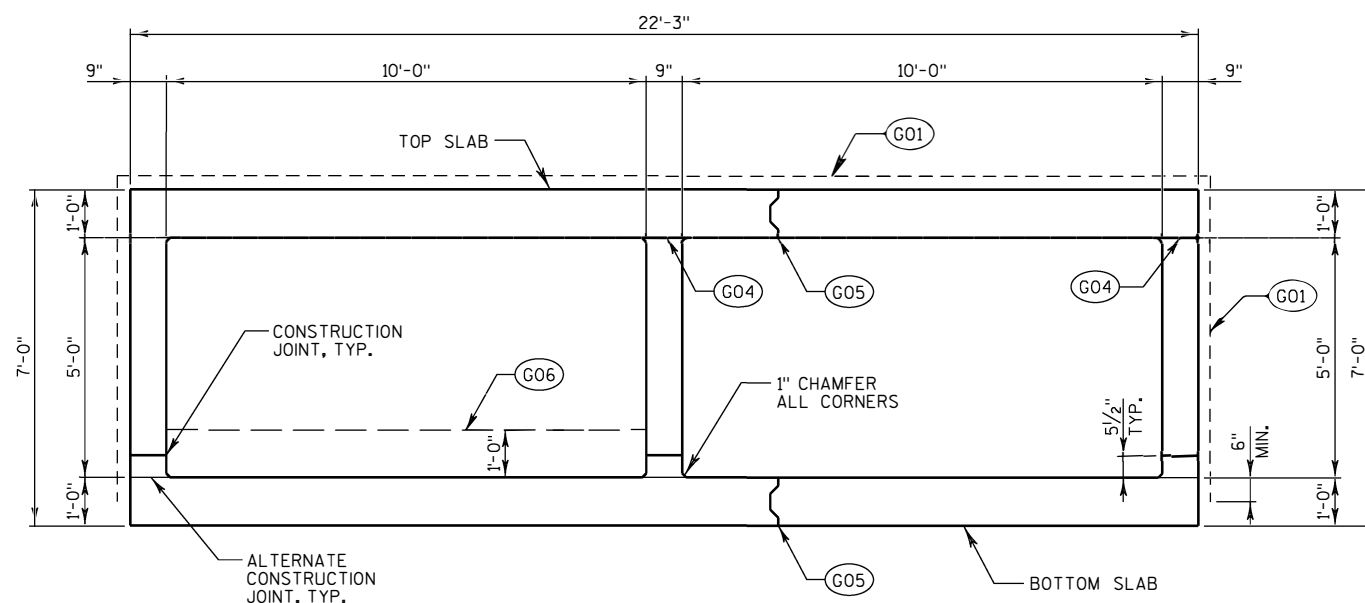
WITHIN THE LENGTH OF THE BOX ALL SPACES EXCAVATED AND NOT OCCUPIED BY THE NEW STRUCTURE SHALL BE BACKFILLED WITH STRUCTURE BACKFILL TO THE ELEVATION AND SECTION EXISTING PRIOR TO EXCAVATION WITHIN THE LENGTH OF THE CULVERT.

THE CONCRETE IN THE CUT OFF WALL MAY BE PLACED UNDERWATER IF THE EXCAVATION CANNOT BE DEWATERED.

THE ALTERNATE CUT OFF WALL MAY BE USED IN LIEU OF THE CAST-IN-PLACE CONCRETE CUT OFF WALLS. PAYMENT SHALL BE BASED ON CONCRETE CUT OFF WALLS.

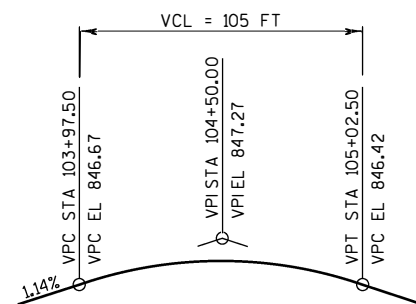
THE CONTRACTOR MAY FURNISH PRECAST CONCRETE BOX CULVERT BARREL SECTIONS, WINGWALLS, HEADERS, AND CUT OFF WALLS IN LIEU OF THE CAST-IN-PLACE BOX CULVERT WITH THE ACCEPTANCE OF THE SHOP DRAWINGS BY THE STRUCTURES MAINTENANCE SECTION. THE PRECAST CONCRETE BOX CULVERT SHALL CONFORM TO PRECAST DETAILS IN CHAPTER 36 STANDARDS OF THE CURRENT WISCONSIN DOT BRIDGE MANUAL. PAYMENT FOR THE PRECAST CULVERT SHALL BE BASED ON THE QUANTITIES AND PRICES BID FOR THE ITEMS LISTED IN THE "TOTAL ESTIMATED QUANTITIES".

ALL PRECAST BOX SECTIONS SHALL BE PLACED ON A BEDDING OF "BACKFILL STRUCTURE TYPE B" OR 6" MINIMUM DEPTH.

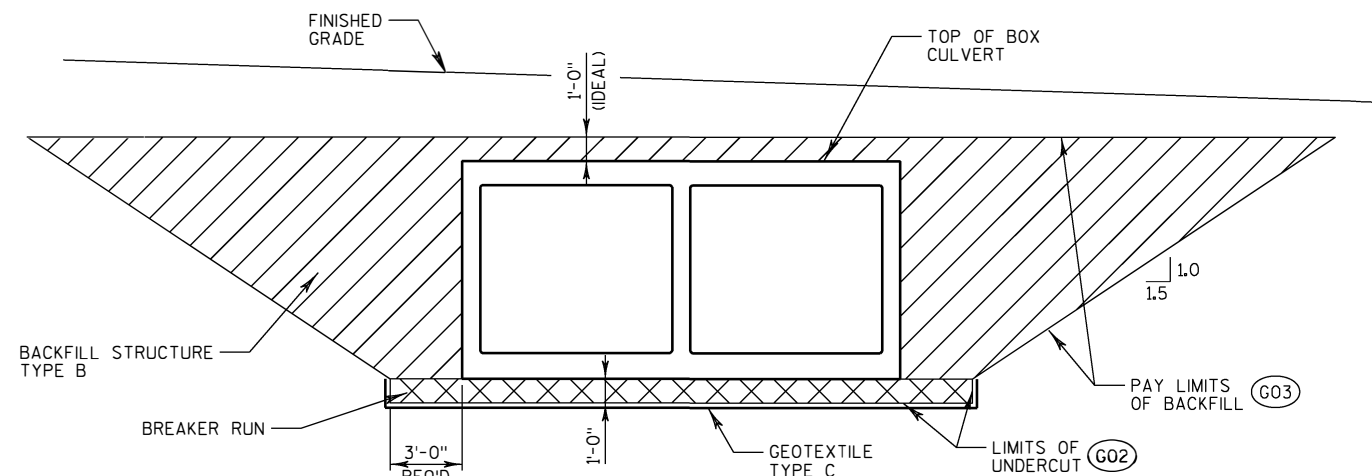


TYPICAL SECTION THRU CULVERT

(LOOKING NORTH)



PROFILE GRADE LINE

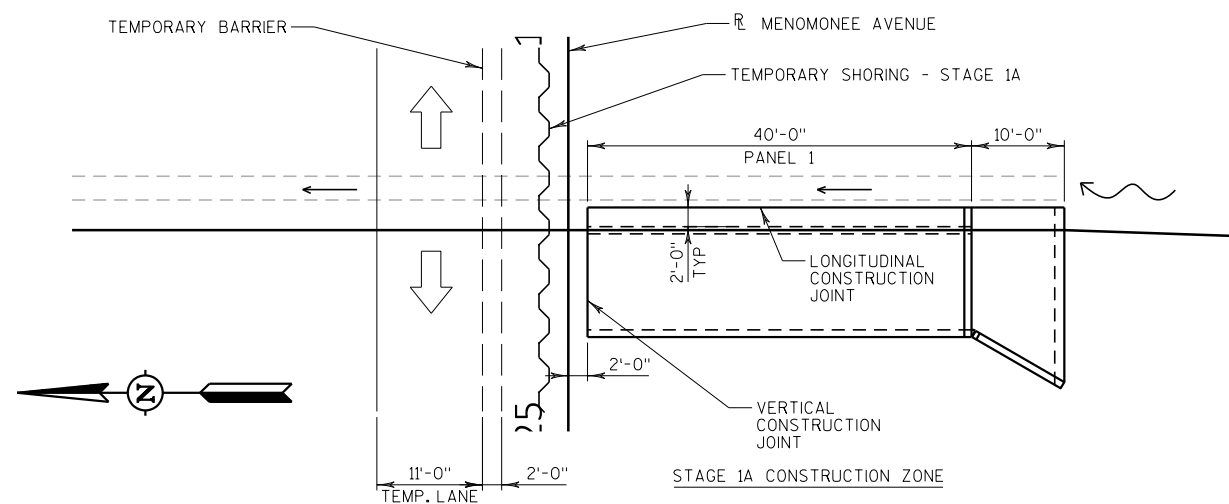


BACKFILL PAY LIMITS

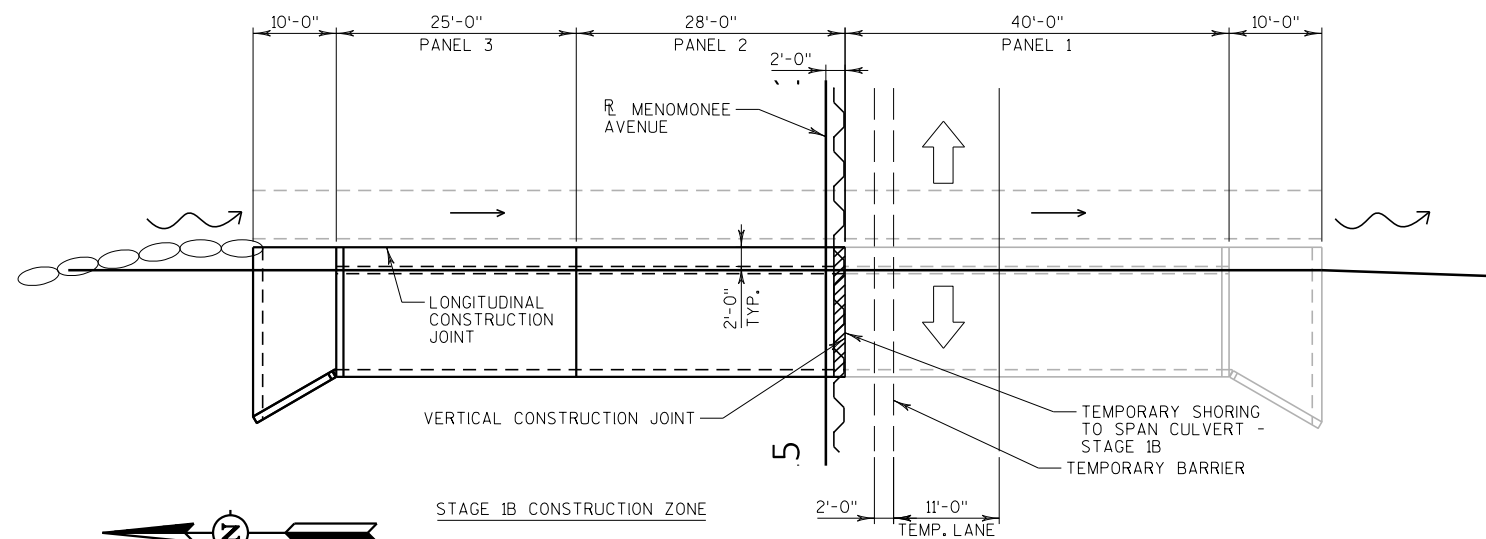
NOTES

- (G01) VERTICAL CONSTRUCTION JOINT WITH 18" MIN. WIDTH RUBBERIZED MEMBRANE WATERPROOFING UP WALLS AND ACROSS TOP SLAB AT VERTICAL CONST. JOINTS. EXTEND 6" MIN. BELOW TOP OF BOTTOM SLAB.
- (G02) UNDER CUT 1'-0". EXCAVATION FOR UNDER CUT TO BE INCLUDED IN EXCAVATION FOR STRUCTURES. PLACE "GEOTEXTILE TYPE C" AND BACKFILL WITH "BREAKER RUN". EXTEND 3'-0" BEYOND THE FOOTPRINT OF THE CULVERT.
- (G03) BACKFILL PAY LIMITS. BACKFILL BEYOND BACKFILL PAY LIMITS SHALL BE INCIDENTAL TO EXCAVATION FOR STRUCTURES. LIMITS OF EXCAVATION SHALL BE DETERMINED BY THE CONTRACTOR.
- (G04) OPTIONAL CONSTRUCTION JOINT. OMIT 1" FILLET IF OPTIONAL CONSTRUCTION JOINT IS USED.
- (G05) LONGITUDINAL CONSTRUCTION JOINT. SEE SHEET 5.
- (G06) PROVIDE "BOX CULVERT FILL MATERIAL" 1'-0" IN DEPTH IN WEST CELL AND APRONS.

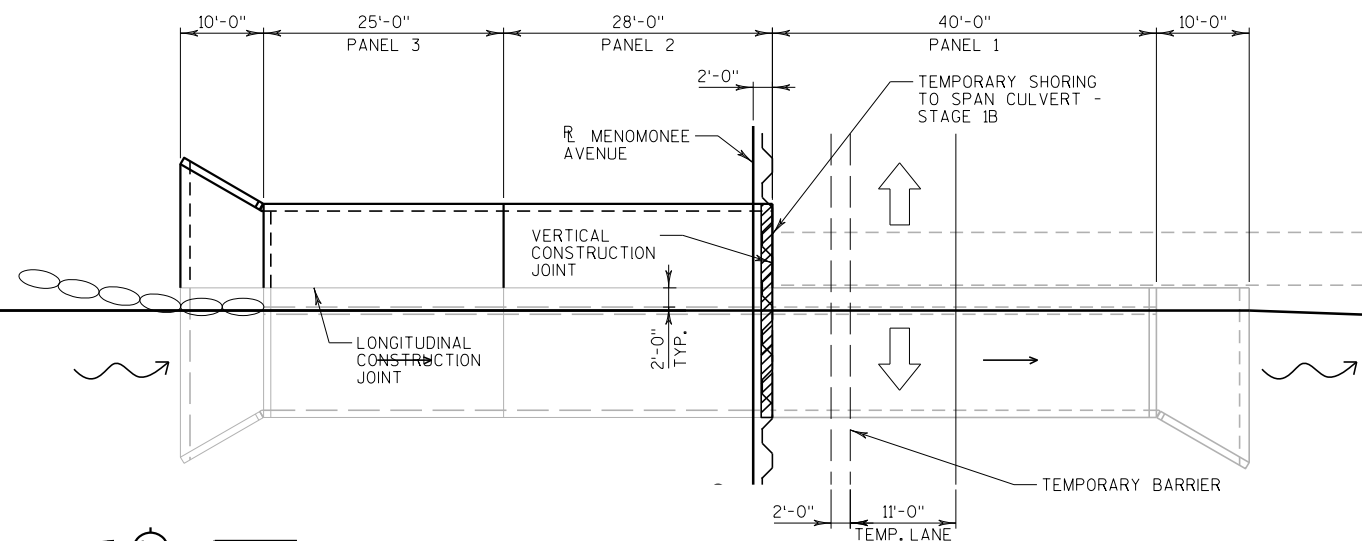
NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-67-401			
DRAWN BY NLD		PLANS CK'D. HJL	
SECTION, DETAILS & QUANTITIES			SHEET 2 OF 8



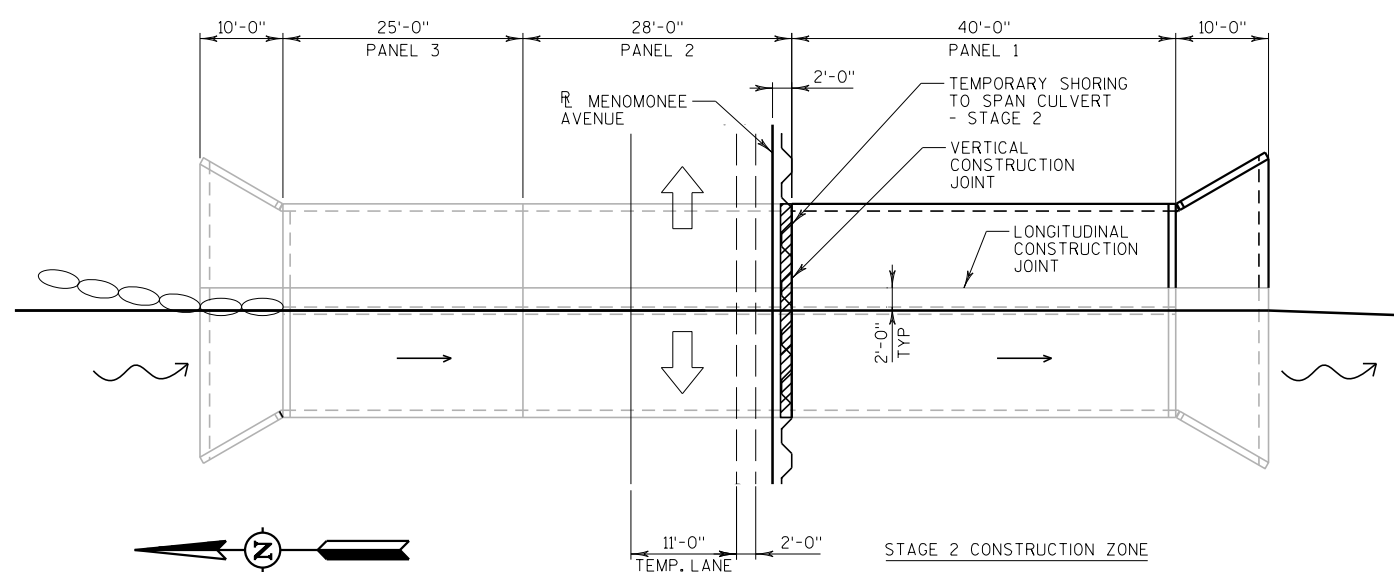
STAGE 1A



STAGE 1B



STAGE 1B



STAGE 2

8

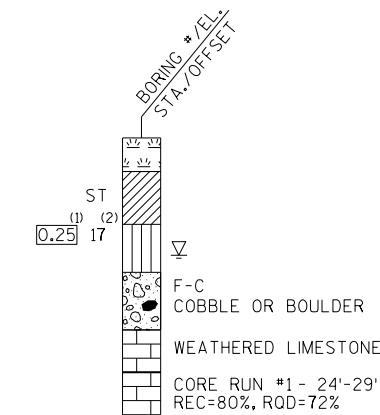
8

NO.	DATE	REVISION
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION		
STRUCTURE B-67-401		
DRAWN BY	NLD	PLANS CK'D. HJL
STAGING DETAILS		SHEET 3 OF 8

MATERIAL SYMBOLS

	ASPHALT		TOPSOIL		PEAT
	CONCRETE		FILL		GRAVEL
	SAND		CLAY		SILT
	BOULDERS OR COBBLES		LIMESTONE		BEDROCK (UNKNOWN)
	SHALE		SANDSTONE		IGNEOUS/META

LEGEND OF BORING



(1) UNCONFINED STRENGTH, AS DETERMINED BY A POCKET PENETROMETER (TSF)

(2) UNLESS OTHERWISE SPECIFIED THE SPT 'N' VALUE IS BASED ON AASHTO T-206, STANDARD PENETRATION TEST. THE SPT 'N' VALUE PRESENTED HAS NOT BEEN CORRECTED FOR OVERBURDEN PRESSURE OR HAMMER EFFICIENCY.

GROUND WATER ELEVATION

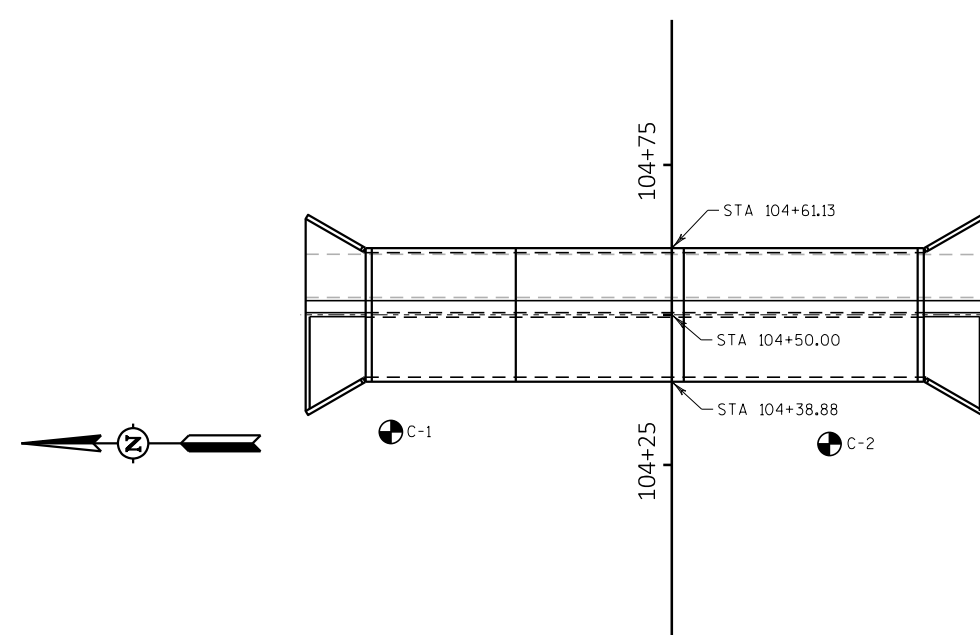
- ▽ AT TIME OF DRILLING
- ▽ END OF DRILLING
- ▽ AFTER DRILLING

ABBREVIATIONS

F-FINE M-MEDIUM C-COARSE ST-SHELBY TUBE

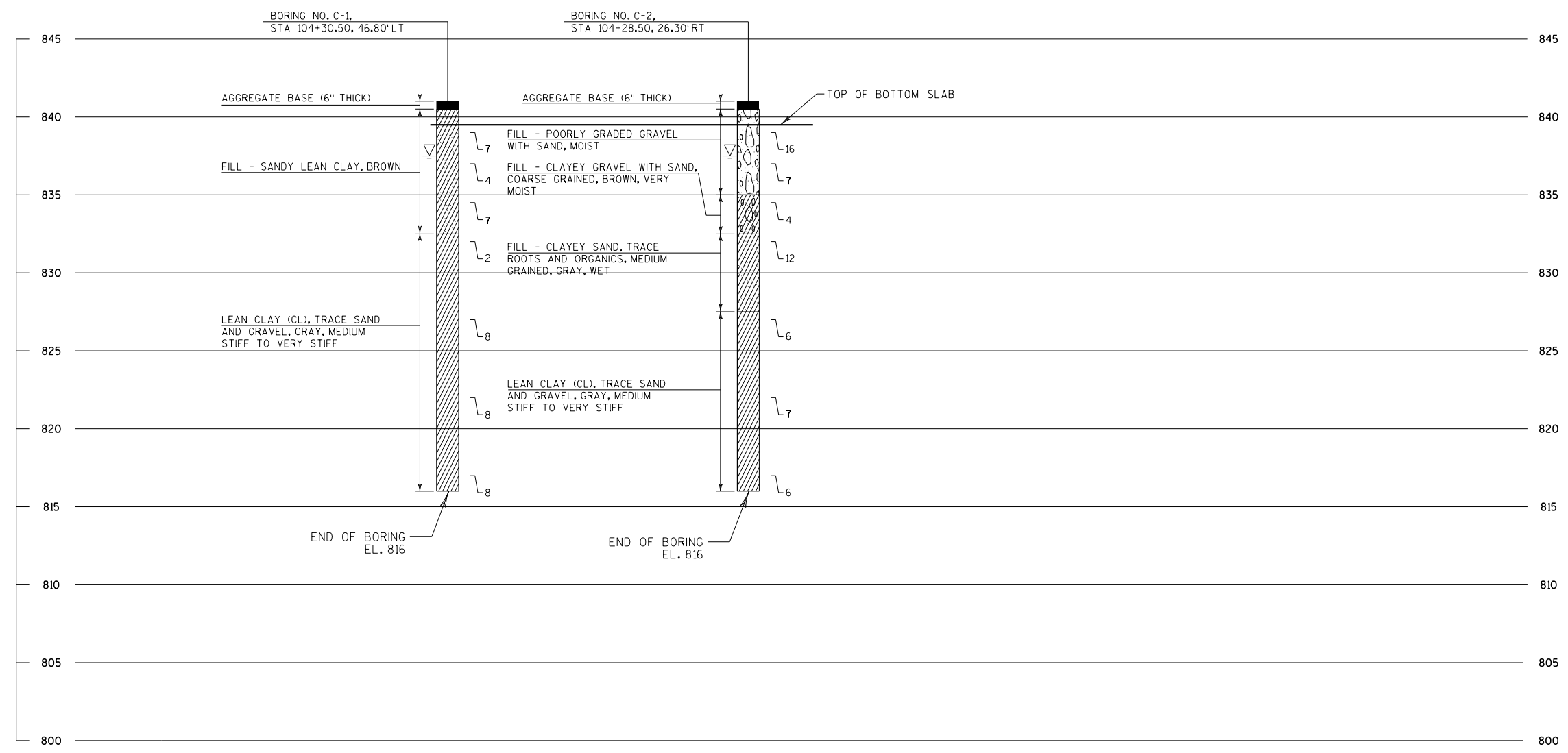
SUBSURFACE EXPLORATION FOR FOUNDATION DESIGN AND BIDDERS INFORMATION

BORINGS WERE COMPLETED AT POINTS APPROXIMATELY AS INDICATED ON THIS DRAWING TO OBTAIN INFORMATION CONCERNING THE CHARACTER OF SUBSURFACE MATERIALS FOUND AT THE SITE. BECAUSE THE INVESTIGATED DEPTHS ARE LIMITED AND THE AREA OF THE BORINGS IS VERY SMALL IN RELATION TO THE ENTIRE SITE, THE WISCONSIN DEPARTMENT OF TRANSPORTATION DOES NOT WARRANT SIMILAR SUBSURFACE CONDITIONS BELOW, BETWEEN, OR BEYOND THESE BORINGS. VARIATIONS IN SOIL CONDITIONS SHOULD BE EXPECTED AND FLUCTUATIONS IN GROUNDWATER LEVELS MAY OCCUR.



BORING #	DATE COMPLETED	LATITUDE	LONGITUDE
C-1	02-05-2024	43.1703°	-88.1320°
C-2	02-05-2024	43.1701°	-88.1320°

BORINGS COMPLETED BY: TERRACON
 REPORT COMPLETED BY: TERRACON
 ALL COORDINATES REFERENCED TO WISCONSIN STATE PLAN COORDINATE SYSTEM, NAD27 SOUTH ZONE



8

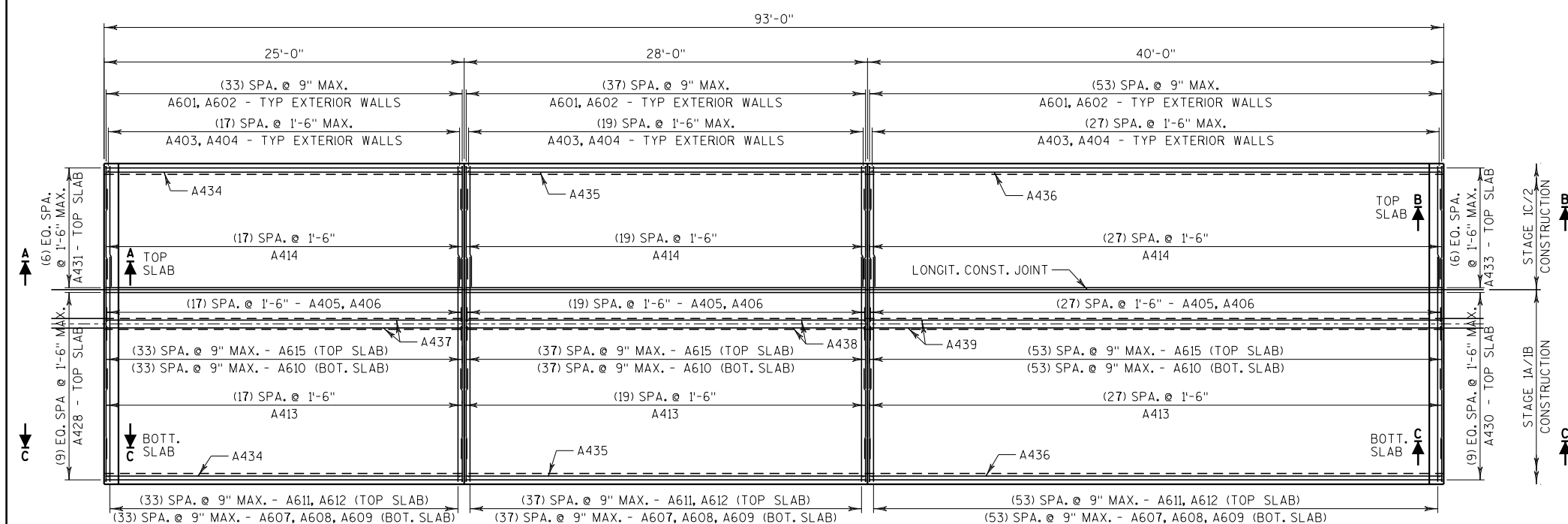
8

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-67-401			
DRAWN BY NLD		PLANS CK'D. HJL	
SUBSURFACE EXPLORATION			SHEET 4 OF 8

BILL OF BARS - BARREL

BAR MARK	CO ₂ /1	NO. REQ'D.	LENGTH	BENT	BAR SERIES	LOCATION
A601		252	7'-3"	X		BOX VERTICAL - CORNER - BOTTOM
A602		252	7'-2"	X		BOX VERTICAL - CORNER - TOP
A403		132	3'-11"			BOX VERTICAL - EXTERIOR WALL
A404		132	3'-11"			BOX VERTICAL - EXTERIOR WALL
A405		132	3'-5"			BOX VERTICAL - INTERIOR WALL
A406		132	5'-11"	X		BOX VERTICAL - INTERIOR WALL
A607		126	13'-0"			BOX HORIZONTAL - BOTTOM SLAB - TOP
A608		126	8'-4"			BOX HORIZONTAL - BOTTOM SLAB - TOP
A609		126	4'-11"			BOX HORIZONTAL - BOTTOM SLAB - TOP
A610		126	9'-0"			BOX HORIZONTAL - BOTTOM SLAB - BOTTOM
A611		126	15'-8"			BOX HORIZONTAL - TOP SLAB - BOTTOM
A612		126	8'-4"			BOX HORIZONTAL - TOP SLAB - BOTTOM
A413		66	6'-4"			BOX HORIZONTAL - TOP SLAB - TOP
A414		66	6'-4"			BOX HORIZONTAL - TOP SLAB - TOP
A615		126	9'-0"			BOX HORIZONTAL - TOP SLAB - TOP
A416		8	24'-8"			BOX HORIZONTAL - BOTTOM CORNER
A417		8	27'-8"			BOX HORIZONTAL - BOTTOM CORNER
A418		8	39'-8"			BOX HORIZONTAL - BOTTOM CORNER
A419		7	24'-8"			BOX HORIZONTAL - BOTTOM SLAB - BOTTOM
A420		7	27'-8"			BOX HORIZONTAL - BOTTOM SLAB - BOTTOM
A421		7	39'-8"			BOX HORIZONTAL - BOTTOM SLAB - BOTTOM
A422		9	24'-8"			BOX HORIZONTAL - BOTTOM SLAB - TOP
A423		9	27'-8"			BOX HORIZONTAL - BOTTOM SLAB - TOP
A424		9	39'-8"			BOX HORIZONTAL - BOTTOM SLAB - TOP
A425		7	24'-8"			BOX HORIZONTAL - BOTTOM SLAB - TOP
A426		7	27'-8"			BOX HORIZONTAL - BOTTOM SLAB - TOP
A427		7	39'-8"			BOX HORIZONTAL - BOTTOM SLAB - TOP
A428		10	24'-8"			BOX HORIZONTAL - TOP SLAB - TOP AND BOTTOM
A429		10	27'-8"			BOX HORIZONTAL - TOP SLAB - TOP AND BOTTOM
A430		10	39'-8"			BOX HORIZONTAL - TOP SLAB - TOP AND BOTTOM
A431		7	24'-8"			BOX HORIZONTAL - TOP SLAB - TOP AND BOTTOM
A432		7	27'-8"			BOX HORIZONTAL - TOP SLAB - TOP AND BOTTOM
A433		7	39'-8"			BOX HORIZONTAL - TOP SLAB - TOP AND BOTTOM
A434		9	24'-8"			BOX HORIZONTAL - EXTERIOR WALL
A435		9	27'-8"			BOX HORIZONTAL - EXTERIOR WALL
A436		9	39'-8"			BOX HORIZONTAL - EXTERIOR WALL
A437		10	24'-8"			BOX HORIZONTAL - INTERIOR WALL
A438		10	27'-8"			BOX HORIZONTAL - INTERIOR WALL
A439		10	39'-8"			BOX HORIZONTAL - INTERIOR WALL
A540		128	4'-0"			VERTICAL JOINT - DOWEL
A341		36	3'-1"	X		INLET HEADER STIRRUPS
A442		4	15'-8"			HEADER HORIZONTAL
A443		4	8'-4"			HEADER HORIZONTAL
A344		36	3'-5"	X		OUTLET HEADER STIRRUPS
A545		48	4'-0"			APRON DOWEL

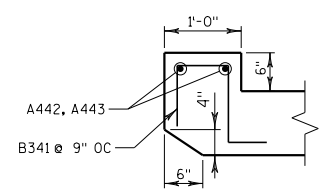
THE FIRST DIGIT OF A BAR MARK SIGNIFIES THE BAR SIZE.
ALL BAR BEND DIMENSIONS ARE OUT TO OUT OF BAR.



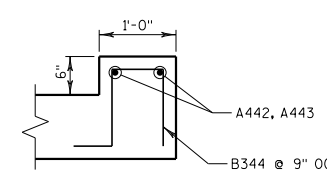
PLAN VIEW OF PANELS

LEGEND

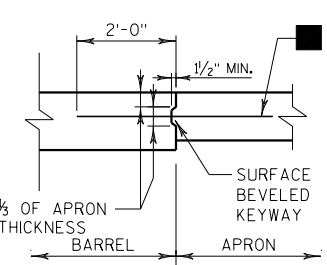
- 18" MIN. WIDTH RUBBERIZED MEMBRANE WATERPROOFING, PLACE ALONG LONGIT. CONST. JOINT IN TOP SLAB FULL LENGTH OF BOX.
- LONGIT. CONST. JOINT (TYP.) FORMED BY SURFACED BEVELED KEYWAY 1/2 OF SLAB THICKNESS
- OPTIONAL CONSTRUCTION JOINT. OMIT 1" CHAMFER IF OPTIONAL JOINT IS USED, TYP.
- ▲ 18" MIN. WIDTH RUBBERIZED MEMBRANE WATERPROOFING UP WALLS AND ACROSS TOP SLAB AT VERT. CONST. JOINTS. EXTEND 6" MIN. BELOW TOP OF BOTTOM SLAB.
- A545 @ 1'-0". CONTRACTOR MAY UTILIZE ADHESIVE ANCHORS NO. 5 BAR, EMBED 6" IN CONCRETE. NO. 5 BAR AND ADHESIVE INCIDENTAL TO "BAR STEEL REINFORCEMENT HS STRUCTURES"
- PROVIDE "....." (1'-0" DEPTH) ENTIRE LENGTH OF CULVERT IN NORTH CELL BETWEEN WEIR WALLS



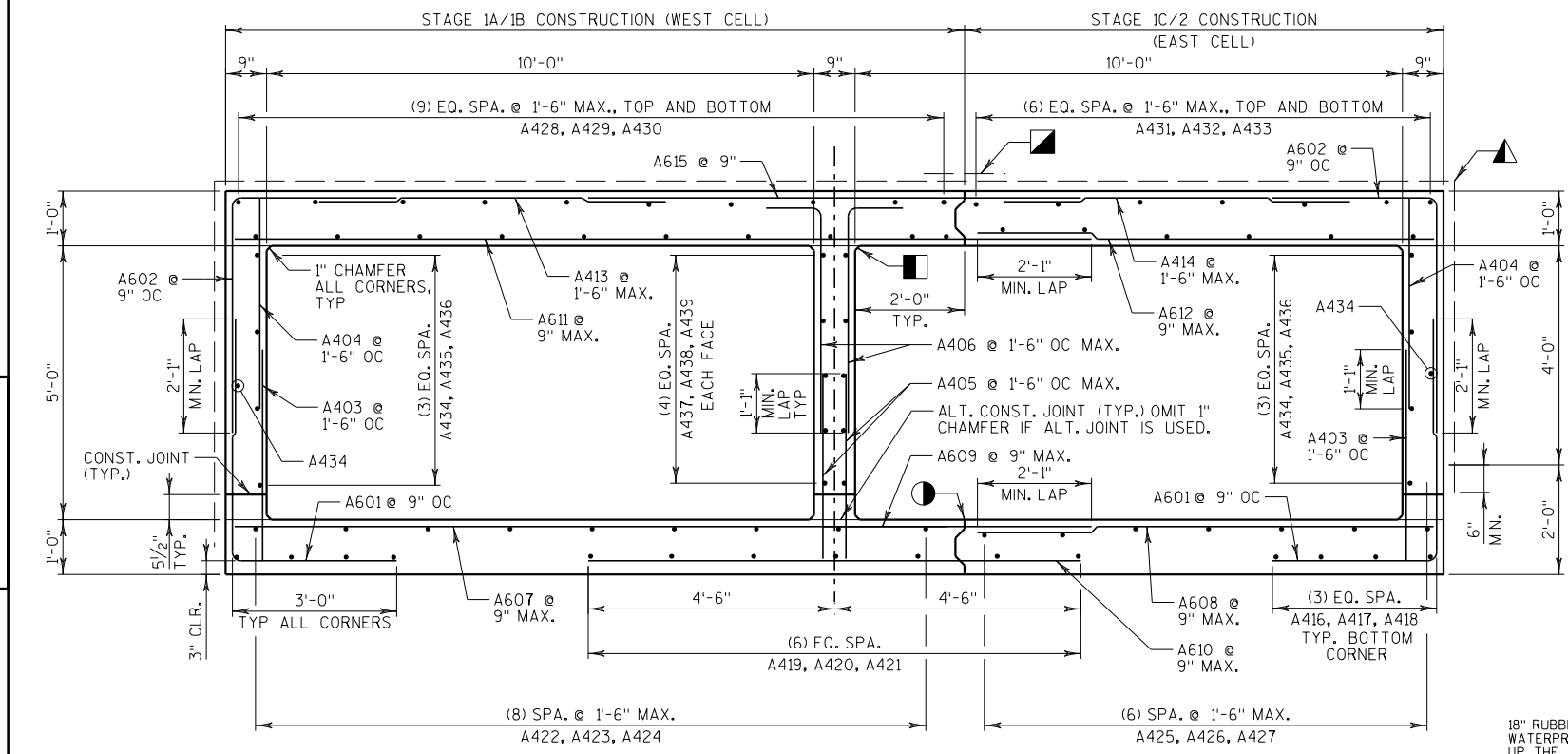
A-A: INLET HEADER DETAIL



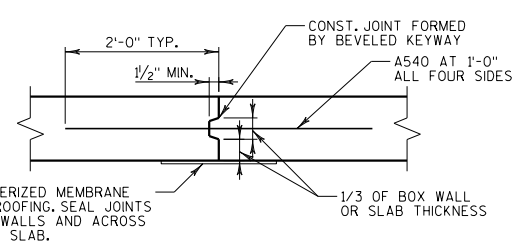
B-B: OUTLET HEADER DETAIL



C-C: APRON-TO-SLAB CONNECTION

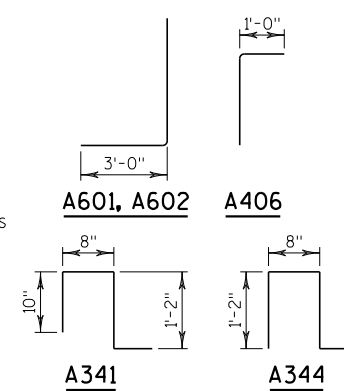


TYPICAL SECTION THROUGH BOX

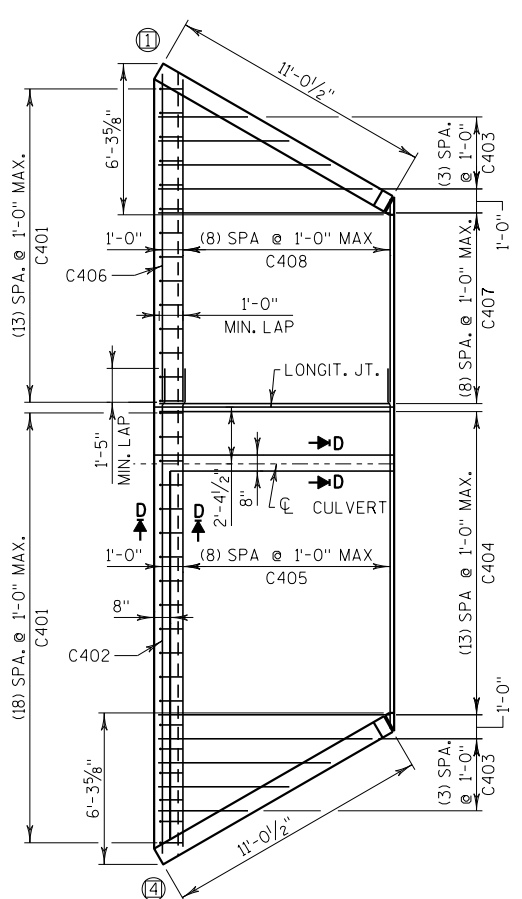


VERTICAL CONSTRUCTION JOINT

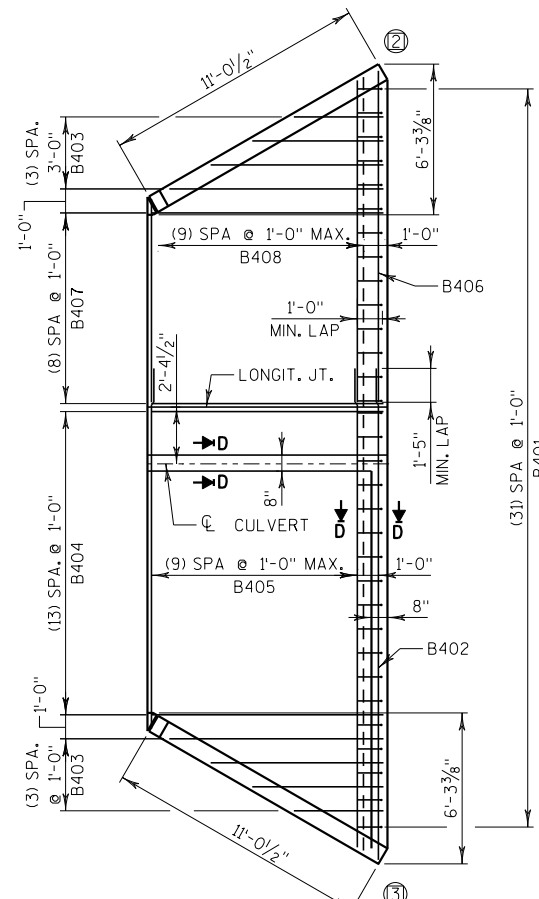
IN LIEU OF CONSTRUCTION JOINTS IN THE BOTTOM SLAB, THE CONTRACTOR MAY USE 2" DEEP SAW CUTS WITHIN 12 HOURS AFTER POURING. B509 BARS AT 1'-0" REQUIRED.



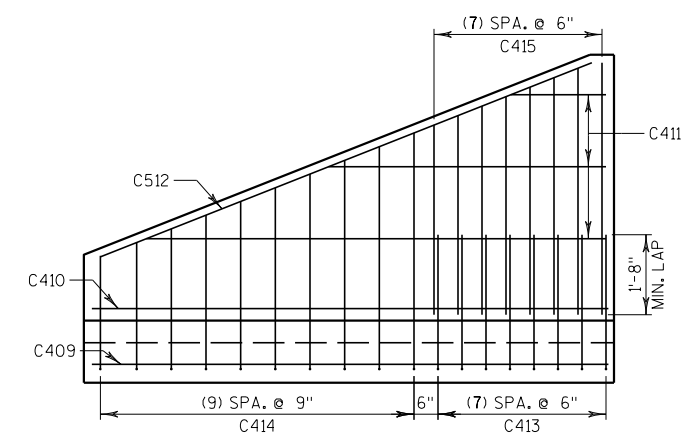
NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
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DRAWN BY		NLD	PLANS CKD. HJL
BOX DETAILS			SHEET 5 OF 8



INLET APRON PLAN

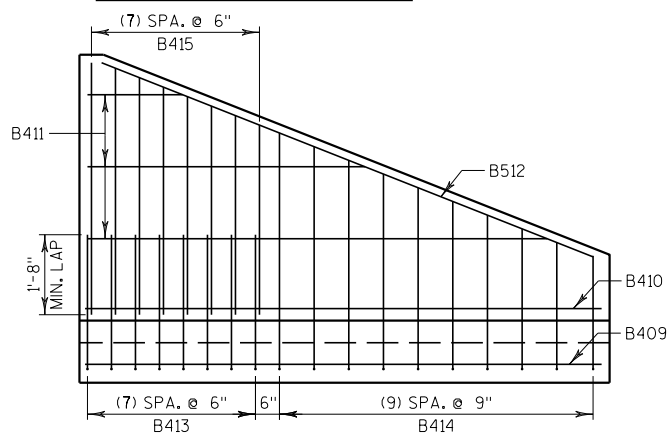


OUTLET APRON PLAN



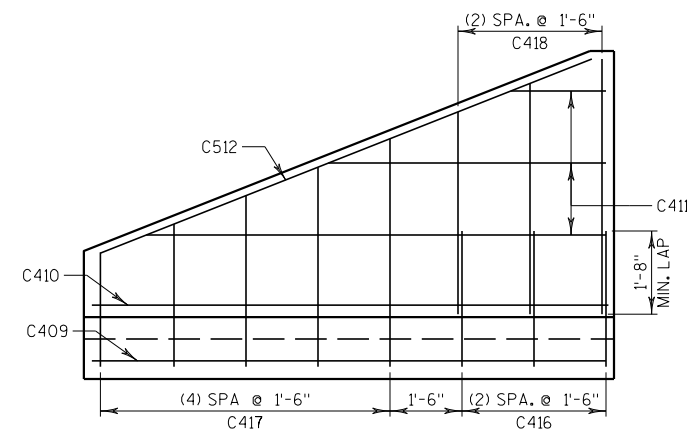
WINGS 1 & 4

SHOWING B.F. REINFORCEMENT



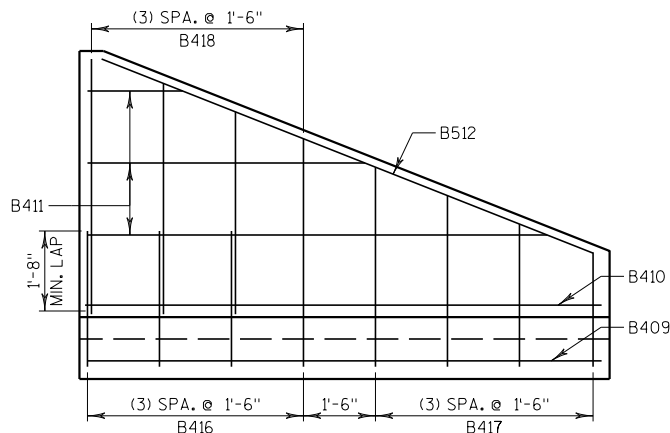
WINGS 2 & 3

SHOWING B.F. REINFORCEMENT



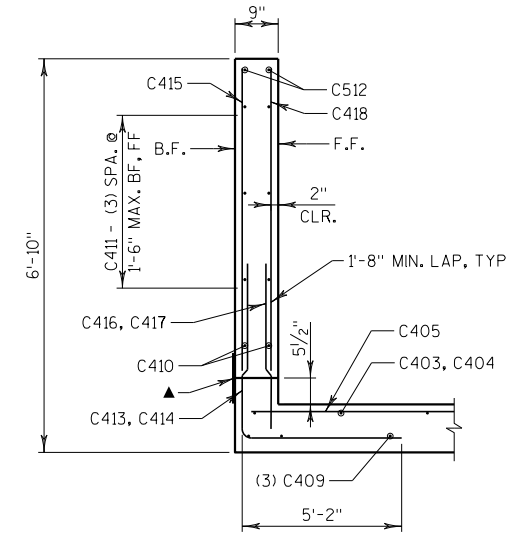
WINGS 1 & 4

SHOWING F.F. REINFORCEMENT

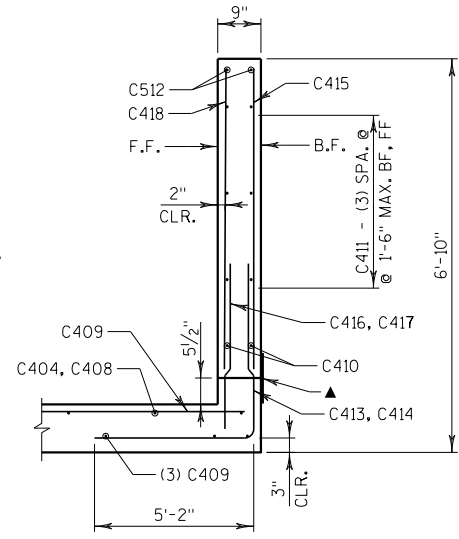


WINGS 2 & 3

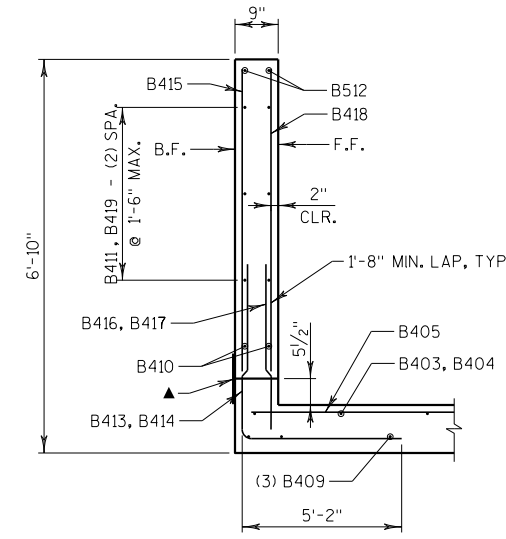
SHOWING F.F. REINFORCEMENT



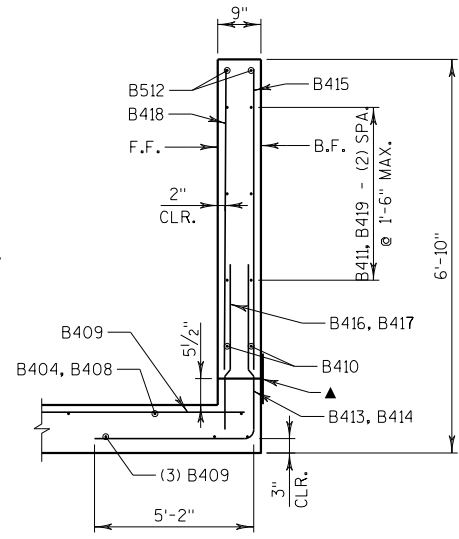
WING 1 SECTION



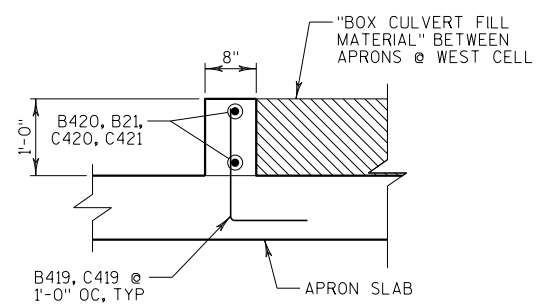
WING 4 SECTION



WING 2 SECTION



WING 3 SECTION



SECTION D-D

LEGEND

▲ HORIZONTAL CONSTRUCTION JOINT. PROVIDE 18" MIN. WIDTH RUBBERIZED MEMBRANE WATERPROOFING ALONG HORIZONTAL CONSTRUCTION JOINT IN WING.

NO.	DATE	REVISION	BY
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APRON			SHEET 6 OF 8

BILL OF BARS - WING AND APRON

STATE PROJECT NUMBER

2720-07-71

BAR MARK	COY	NO. REQ'D.	LENGTH	BENT	BAR SERIES	LOCATION
B401		33	5'-3"	X		OUTLET CUTOFF WALL - VERT
B402		6	20'-3"			OUTLET CUTOFF WALL - HORIZ
B403		8	6'-4"		X	APRON HORIZONTAL - TOP
B404		14	9'-8"			APRON HORIZONTAL - TOP
B405		9	17'-7"	X		APRON HORIZONTAL - TOP
B406		6	13'-8"			OUTLET CUTOFF WALL - HORIZ
B407		9	9'-8"			APRON HORIZONTAL - TOP
B408		9	11'-0"	X		APRON HORIZONTAL - TOP
B409		6	10'-8"			APRON HORIZONTAL - BOTTOM
B410	X	4	10'-8"			WINGWALL - HORIZONTAL
B411	X	12	5'-9"		X	WINGWALL - HORIZONTAL
B412	X	4	11'-0"			WINGWALL - HORIZONTAL
B413	X	16	7'-11"	X		WINGWALL - VERTICAL
B414	X	20	8'-9"	X	X	WINGWALL - VERTICAL
B415	X	16	4'-7"		X	WINGWALL - VERTICAL
B416	X	6	2'-10"			WINGWALL - VERTICAL
B417	X	10	3'-7"		X	WINGWALL - VERTICAL
B418	X	6	4'-9"		X	WINGWALL - VERTICAL
B419	X	28	2'-4"	X		WEIR WALL DOWEL
B420	X	2	15'-8"			WEIR WALL HORIZONTAL
B421	X	2	9'-8"			WEIR WALL HORIZONTAL
C401		33	5'-3"	X		INLET CUTOFF WALL - VERT
C402		4	20'-3"			INLET CUTOFF WALL - HORIZ
C403		8	6'-4"		X	APRON HORIZONTAL - TOP
C404		14	9'-8"			APRON HORIZONTAL - TOP
C405		9	17'-7"	X		APRON HORIZONTAL - TOP
C406		4	13'-8"			INLET CUTOFF WALL - HORIZ
C407		9	9'-8"			APRON HORIZONTAL - TOP
C408		9	11'-0"	X		APRON HORIZONTAL - TOP
C409		6	10'-8"			APRON HORIZONTAL - BOTTOM
C410	X	4	10'-8"			WINGWALL - HORIZONTAL
C411	X	12	5'-9"		X	WINGWALL - HORIZONTAL
C412	X	4	11'-0"			WINGWALL - HORIZONTAL
C413	X	16	7'-11"	X		WINGWALL - VERTICAL
C414	X	20	8'-9"	X	X	WINGWALL - VERTICAL
C415	X	16	4'-7"		X	WINGWALL - VERTICAL
C416	X	6	2'-10"			WINGWALL - VERTICAL
C417	X	10	3'-7"		X	WINGWALL - VERTICAL
C418	X	6	4'-9"		X	WINGWALL - VERTICAL
C419	X	28	2'-4"	X		WEIR WALL DOWEL
C420	X	2	15'-8"			WEIR WALL HORIZONTAL
C421	X	2	9'-8"			WEIR WALL HORIZONTAL

THE FIRST DIGIT OF A BAR MARK SIGNIFIES THE BAR SIZE.

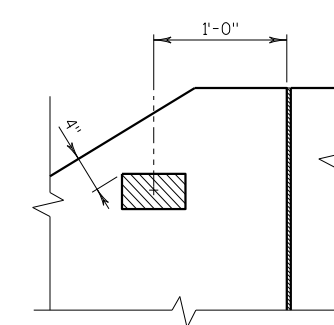
ALL BAR BEND DIMENSIONS ARE OUT TO OUT OF BAR.

LENGTH SHOWN FOR BAR SERIES IS AN AVERAGE LENGTH AND SHOULD ONLY BE USED FOR BAR WEIGHT CALCULATIONS. SEE BAR SERIES TABLE FOR ACTUAL LENGTHS.

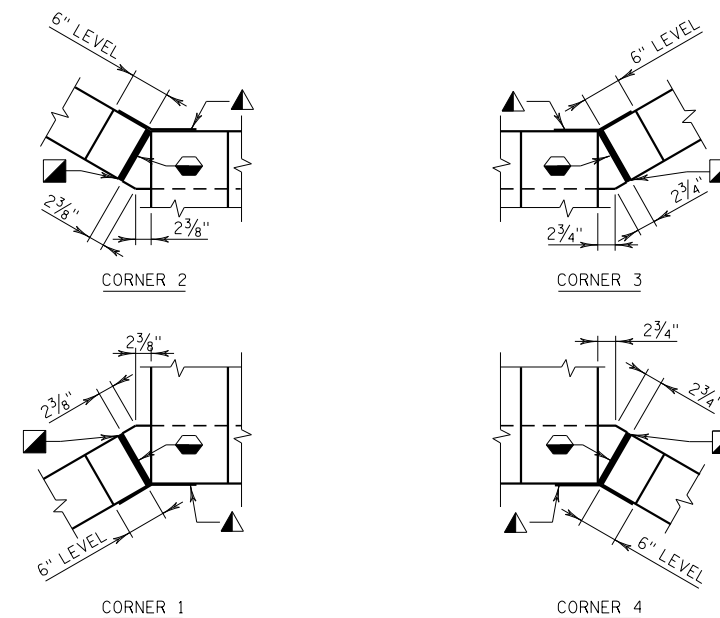
BAR SERIES TABLE

BAR MARK	NO REQ'D.	LENGTH
B403	2 SERIES OF 4	3'-8" TO 8'-11"
B405	1 SERIES OF 9	15'-1" TO 20'-0"
B408	1 SERIES OF 9	8'-6" TO 13'-6"
B411	4 SERIES OF 3	1'-11" TO 9'-6"
B414	2 SERIES OF 8	7'-5" TO 10'-0"
B415	2 SERIES OF 8	3'-11" TO 5'-3"
B417	2 SERIES OF 4	2'-4" TO 4'-9"
B418	2 SERIES OF 3	4'-2" TO 5'-3"

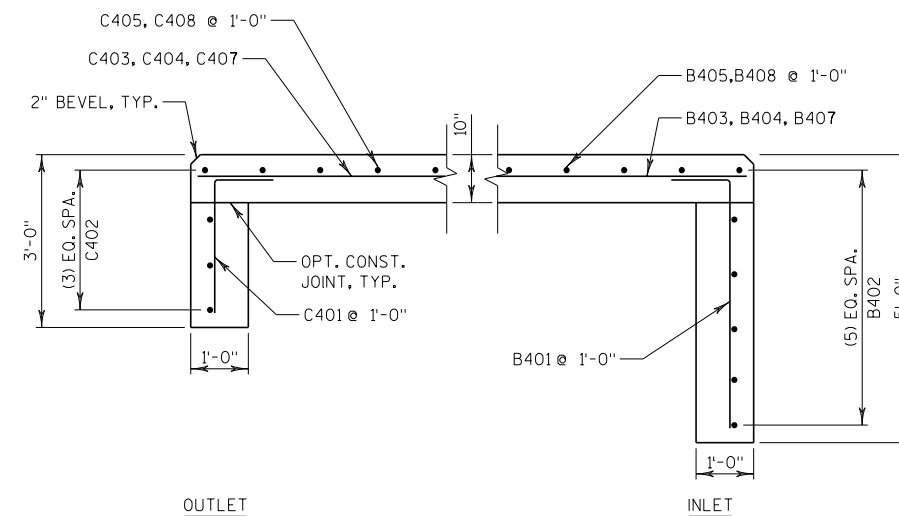
BAR MARK	NO REQ'D.	LENGTH
C403	2 SERIES OF 4	3'-8" TO 8'-11"
C405	1 SERIES OF 9	15'-1" TO 20'-0"
C408	1 SERIES OF 9	8'-6" TO 13'-6"
C411	4 SERIES OF 3	1'-11" TO 9'-6"
C414	2 SERIES OF 8	7'-5" TO 10'-0"
C415	2 SERIES OF 8	3'-11" TO 5'-3"
C417	2 SERIES OF 4	2'-4" TO 4'-9"
C418	2 SERIES OF 3	4'-2" TO 5'-3"



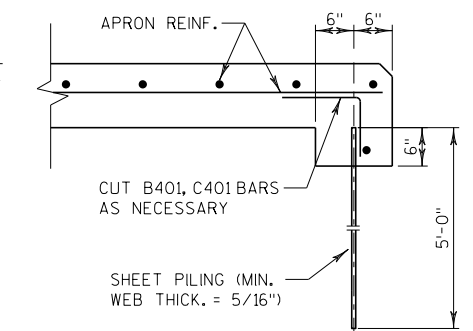
NAME PLATE LOCATION
WING 4



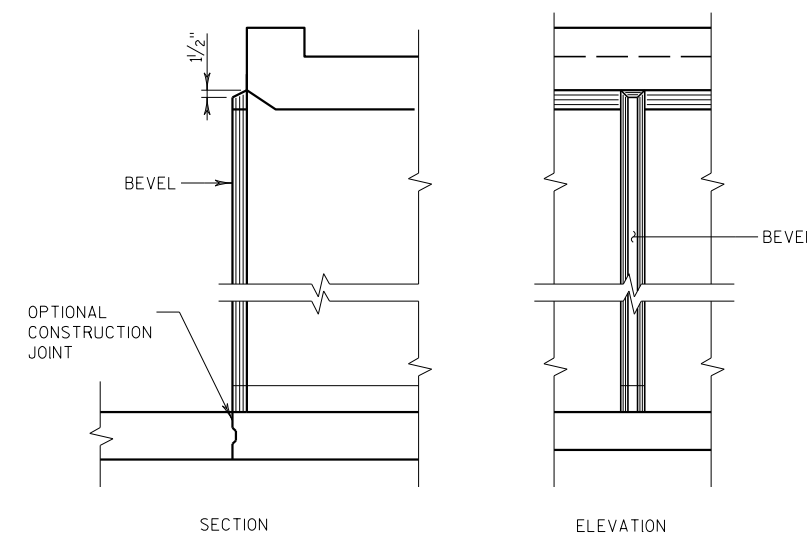
CORNER DETAILS



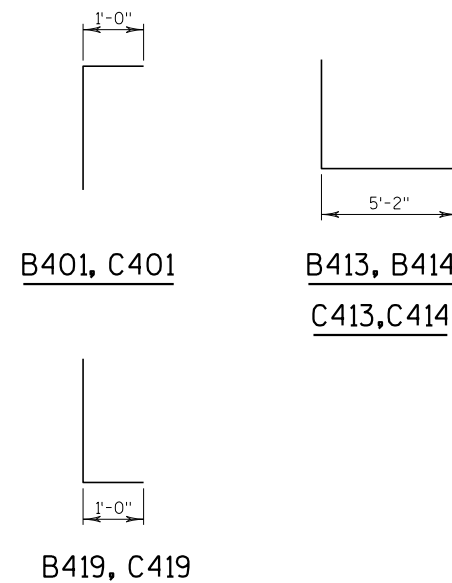
CUTOFF WALLS



ALTERNATE CUTOFF WALL



INLET NOSE DETAILS



B401, C401

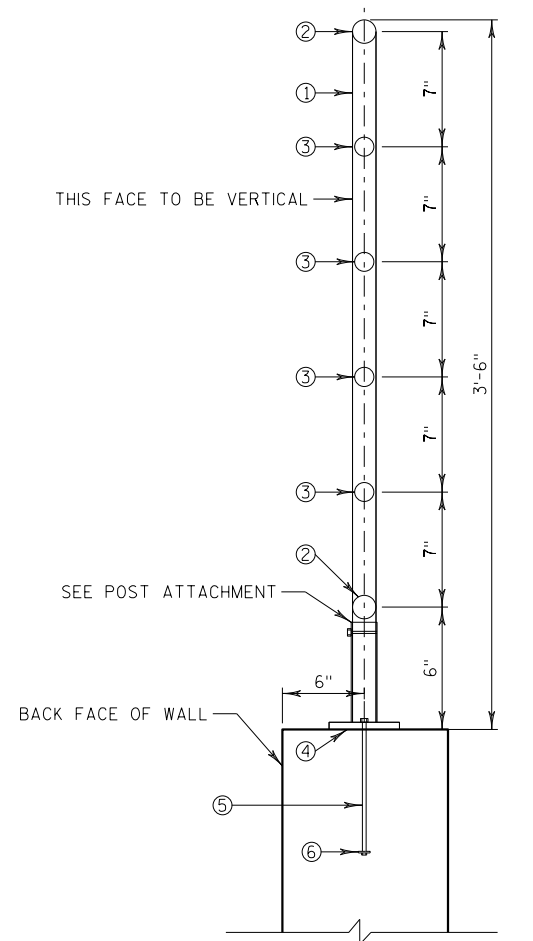
B413, B414
C413, C414

B419, C419

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8

8



SECTION THRU RAILING

NOTES

BID ITEM SHALL BE "RAILING PIPE", WHICH SHALL INCLUDE ALL ITEMS SHOWN.

POST BASE PLATES SHALL BE FLAT WITH ALL SURFACES SMOOTH AND FREE FROM WARP AND ALL EDGES SMOOTH, STRAIGHT, AND VERTICAL. ALL PLATE CUTS SHALL BE MACHINE OR MACHINE FLAME CUT.

ALL MATERIAL EXCEPT ANCHORAGE DETAIL (NO. 10) SHALL BE GALVANIZED AFTER FABRICATION. PRIOR TO GALVANIZING, ALL STEEL RAILING POSTS AND STEEL TUBING SHALL BE GIVEN A NO. 6 BLAST CLEANING BY S.S.P.C. SPECIFICATIONS. PAINT OVER GALVANIZING WITH AN APPROVED TIE COAT AND TOP COAT AS SPECIFIED IN THE CONTRACT DOCUMENTS. THE RAILING SHALL BE PAINTED FEDERAL COLOR NO. 27038, BLACK.

ALL RAILS, POSTS, AND SLEEVES ARE STANDARD WEIGHT PIPE, SCHEDULE 40.

RAILING SHALL BE FABRICATED IN LENGTHS THAT INCLUDE 3 OR 4 POSTS.

ANCHORAGES SHALL BE ACCURATELY PLACED TO PROVIDE CORRECT ALIGNMENT OF RAILING. SET NORMAL TO GRADE.

CUT BOTTOM OF POST TO MAKE POST VERTICAL IN BOTH TRANSVERSE AND LONGITUDINAL DIRECTION.

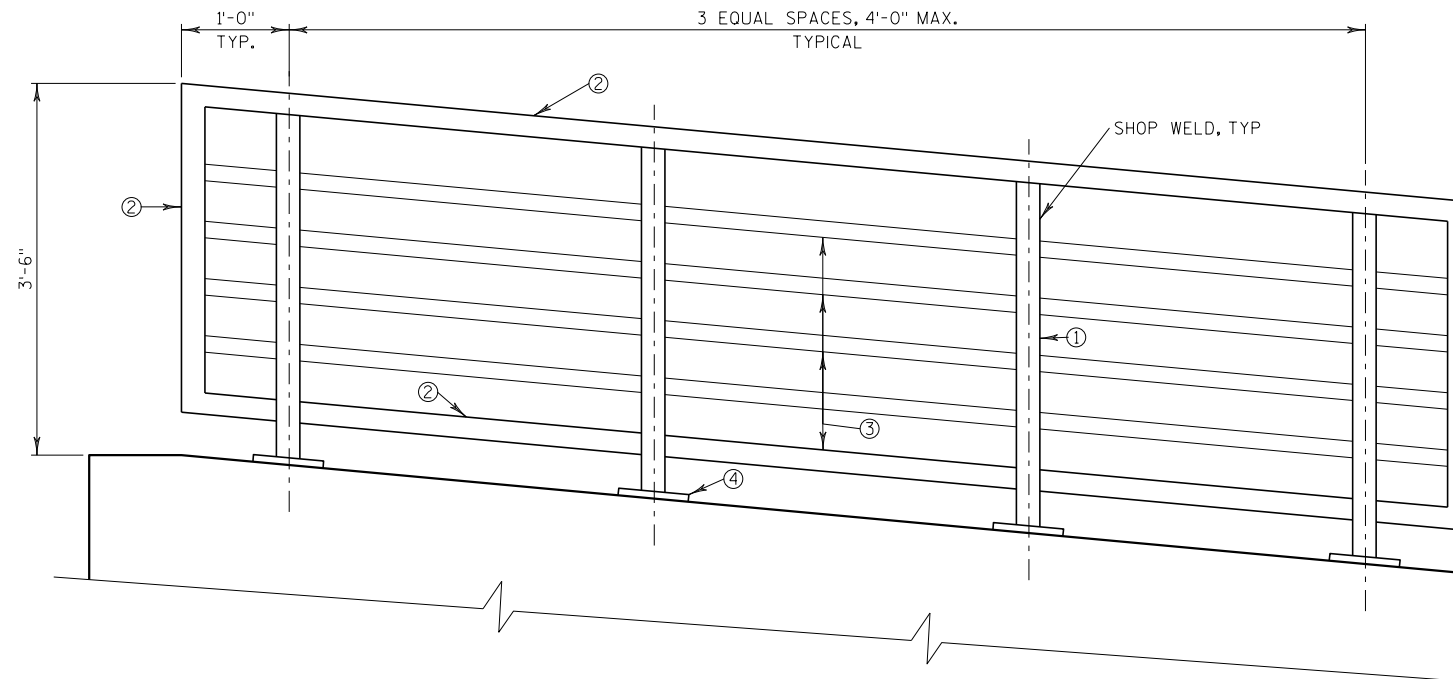
STEEL SHIMS SHALL BE PROVIDED & USED UNDER BASE PLATES WHERE REQUIRED FOR ALIGNMENT.

FILL BOLT SLOT OPENINGS IN SHIMS AND PLATE NO. 8 AND CAULK AROUND PERIMETER OF PLATE NO. 8 WITH NON-STAINING GRAY NON-BITUMINOUS JOINT SEALER.

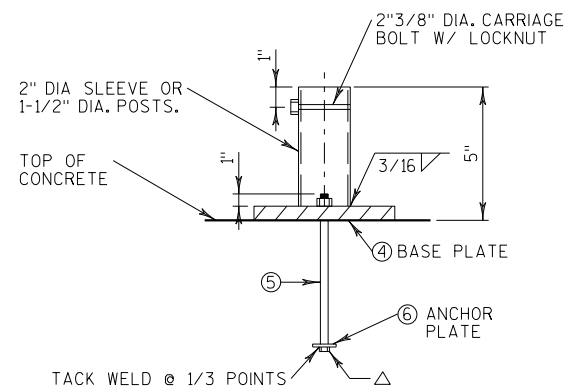
1/4" DIA. VENT HOLES LOCATED IN TOP RAIL OVER RAIL POSTS AND AT LOW END OF OTHER RAILS.

PLACE ALL NUTS ON OUTSIDE OF POSTS.

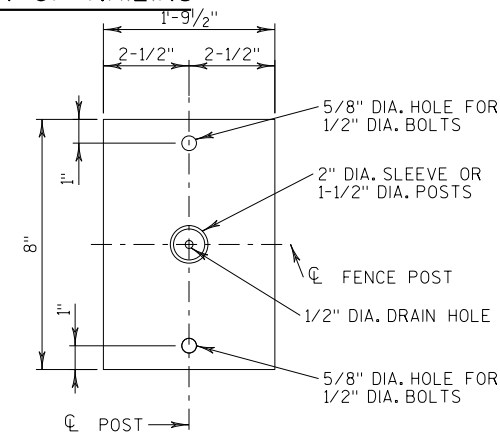
⊗ 1/2" DIA. CONCRETE MASONRY ANCHOR, TYPE "S", 6" EMBEDMENT (EPOXY ANCHORED) MIN. PULLOUT OF 10 KIPS. THREADED LENGTH OF ANCHOR, WASHER, AND NUT SHALL BE GALVANIZED.



ELEVATION OF RAILING

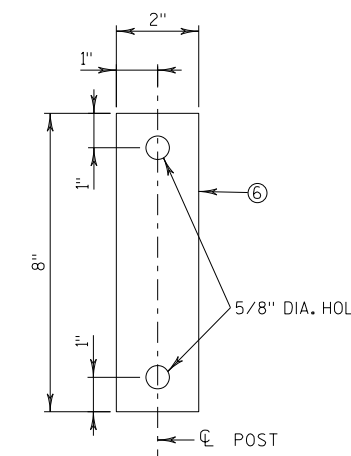


ELEVATION



POST ATTACHMENT

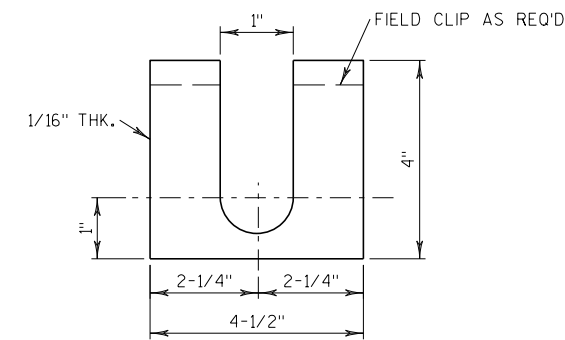
UNIT SHALL BE GALV. AFTER FABRICATION



POST ANCHOR PLATE

LEGEND

- ① 1-1/2" DIA. STEEL PIPE FOR POST. CUT BOTTOM OF POST TO MATCH TOP OF CONCRETE. PLACE POSTS VERTICAL.
- ② 1-1/2" DIA. STEEL PIPE FOR TOP & BOT. RAIL. WELD TO NO. 1.
- ③ 1" DIA STEEL PIPE FOR INTERMEDIATE RAILS. WELD TO NO. 1.
- ④ PLATE 3/4" X 5" X 8" WITH 5/8" DIA. HOLE FOR ANCHOR BOLTS NO. 9. WELD TO NO. 1 OR PIPE SLEEVE AS SHOWN.
- ⑤ A325 - 1/2" DIA. X 6-7/8" LONG HEX BOLTS (GALVANIZED) WITH A325 NUT AND WASHER. 2 REQ'D. PER POST. THREAD 3" AND PLACE NORMAL TO PLATE NO. 8. CHAMFER TOP OF BOLTS BEFORE THREADING.
- ⑥ 1/4" X 2" X 8" FLAT BAR WITH 5/8" DIA. HOLES FOR AN CHOR BOLTS NO. 9.



POST SHIM DETAILS

(PROVIDE 4 SHIMS PER POST)

△ 1/2" DIA. X 6-7/8" LONG GALVANIZED HEX. BOLT WITH NUT AND WASHER. TYPE S. 1/2" DIA. CONCRETE MASONRY ANCHORS MAY BE SUBSTITUTED FOR 1/2" DIA. BOLTS. ANCHOR PLATE NOT REQUIRED WHEN TYPE S ANCHORS ARE USED. SEE ⊗ IN "GENERAL NOTES"

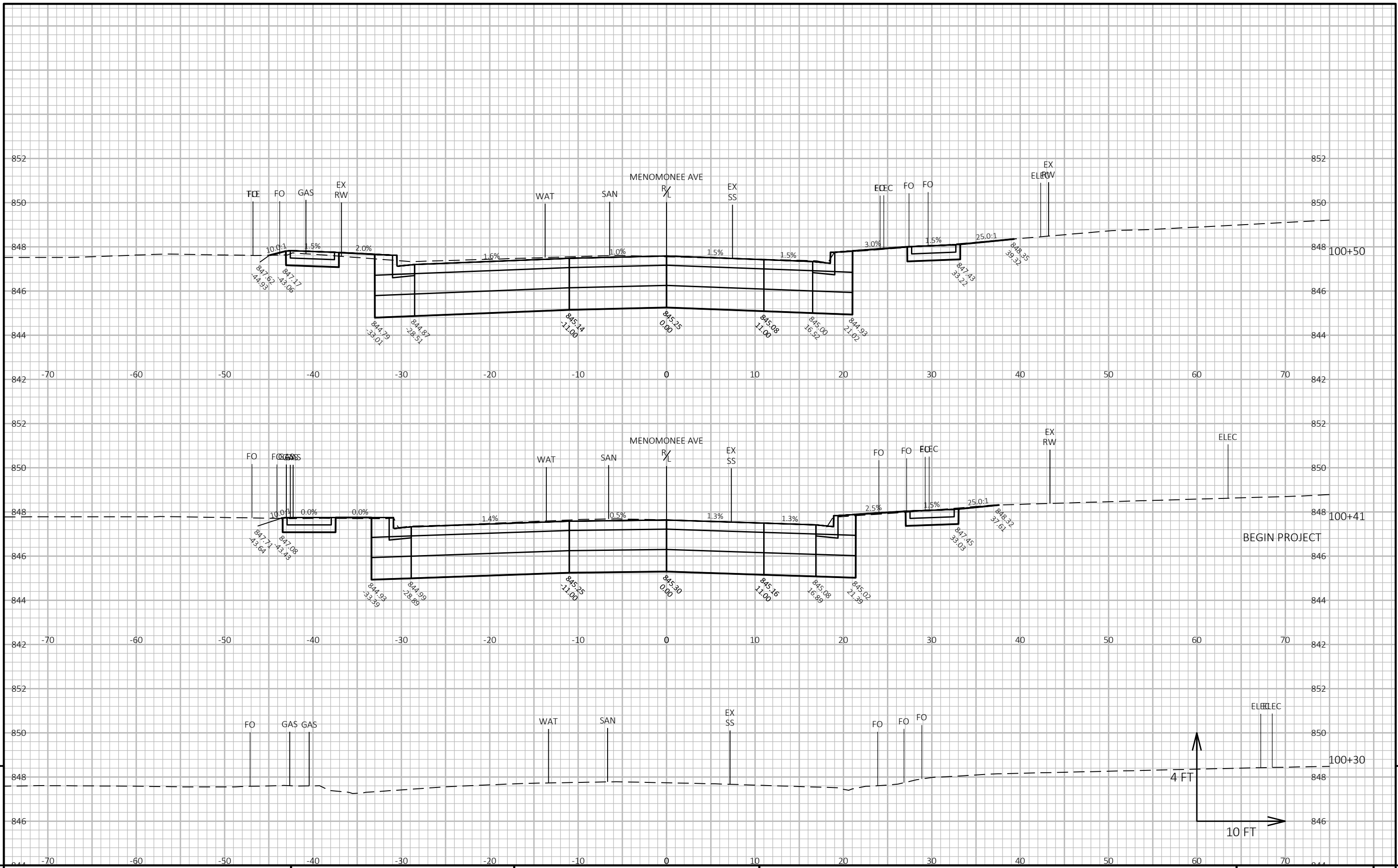
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DRAWN BY		NLD	PLANS CK'D. HJL
RAILING PIPE			SHEET 8 OF 8

STATION	REAL STATION	DISTANCE	AREA (SF)		INCREMENTAL VOL (CY) (UNADJUSTED)		CUMULATIVE VOL (CY)		
			CUT	FILL	CUT	FILL	CUT	EXPANDED FILL	MASS ORDINATE
							1.00	1.25	
100+30	10030.00	0.00	0.00	0.00	0	0	0	0	0
100+41	10040.73	10.73	139.31	0.00	28	0	28	0	28
100+50	10050.00	9.27	137.89	0.00	48	0	76	0	76
101+00	10100.00	50.00	138.18	0.00	256	0	332	0	332
101+50	10150.00	50.00	128.26	0.00	247	0	579	0	579
102+00	10200.00	50.00	141.29	0.00	250	0	829	0	829
102+50	10250.00	50.00	122.34	0.00	244	0	1,073	0	1,073
103+00	10300.00	50.00	118.16	0.00	223	0	1,296	0	1,296
103+50	10350.00	50.00	95.63	0.00	198	0	1,494	0	1,494
103+81	10380.75	30.75	179.76	0.00	157	0	1,651	0	1,651
104+00	10400.00	19.25	179.60	0.00	128	0	1,779	0	1,779
104+50	10450.00	50.00	38.31	0.00	202	0	1,981	0	1,981
105+00	10500.00	50.00	55.37	0.00	87	0	2,068	0	2,068
105+50	10550.00	50.00	96.95	0.00	141	0	2,209	0	2,209
106+00	10600.00	50.00	118.46	0.00	199	0	2,408	0	2,408
106+50	10650.00	50.00	121.60	0.00	222	0	2,630	0	2,630
107+00	10700.00	50.00	225.82	0.00	322	0	2,952	0	2,952
107+04	10703.89	3.89	224.07	0.00	32	0	2,984	0	2,984
107+50	10750.00	46.11	116.57	0.00	291	0	3,275	0	3,275
108+00	10800.00	50.00	126.19	0.00	225	0	3,500	0	3,500
108+50	10850.00	50.00	123.49	0.00	231	0	3,731	0	3,731
109+00	10900.00	50.00	128.09	0.00	233	0	3,964	0	3,964
109+50	10950.00	50.00	115.67	0.00	226	0	4,190	0	4,190
110+00	11000.00	50.00	116.20	0.00	215	0	4,405	0	4,405
110+50	11050.00	50.00	128.92	0.00	227	0	4,632	0	4,632
111+00	11100.00	50.00	121.14	0.00	232	0	4,864	0	4,864
111+50	11150.00	50.00	114.58	0.00	218	0	5,082	0	5,082
112+00	11200.00	50.00	114.00	0.00	212	0	5,294	0	5,294
112+50	11250.00	50.00	110.34	0.00	208	0	5,502	0	5,502
113+00	11300.00	50.00	121.62	0.00	215	0	5,717	0	5,717
113+50	11350.00	50.00	116.98	0.00	221	0	5,938	0	5,938
113+78	11377.60	27.60	197.64	0.00	161	0	6,099	0	6,099
114+00	11400.00	22.40	184.63	0.00	159	0	6,258	0	6,258
114+50	11450.00	50.00	114.58	0.00	277	0	6,535	0	6,535
115+00	11500.00	50.00	118.79	0.00	216	0	6,751	0	6,751
115+50	11550.00	50.00	118.49	0.00	220	0	6,971	0	6,971
116+00	11600.00	50.00	116.87	0.00	218	0	7,189	0	7,189
116+50	11650.00	50.00	102.69	0.00	203	0	7,392	0	7,392
117+00	11700.00	50.00	97.76	0.00	186	0	7,578	0	7,578
117+23	11723.34	23.34	159.23	0.00	111	0	7,689	0	7,689
117+28	11727.91	4.57	159.18	0.00	27	0	7,716	0	7,716
117+50	11750.00	22.09	120.86	0.00	115	0	7,831	0	7,831
118+00	11800.00	50.00	112.01	0.00	216	0	8,047	0	8,047
118+50	11850.00	50.00	121.76	0.00	216	0	8,263	0	8,263
119+00	11900.00	50.00	114.64	0.00	219	0	8,482	0	8,482
119+50	11950.00	50.00	107.96	0.00	206	0	8,688	0	8,688
120+00	12000.00	50.00	107.93	0.00	200	0	8,888	0	8,888
120+46	12045.91	45.91	176.44	0.00	242	0	9,130	0	9,130
120+50	12050.00	4.09	177.49	0.00	27	0	9,157	0	9,157
121+00	12100.00	50.00	141.47	0.00	295	0	9,452	0	9,452

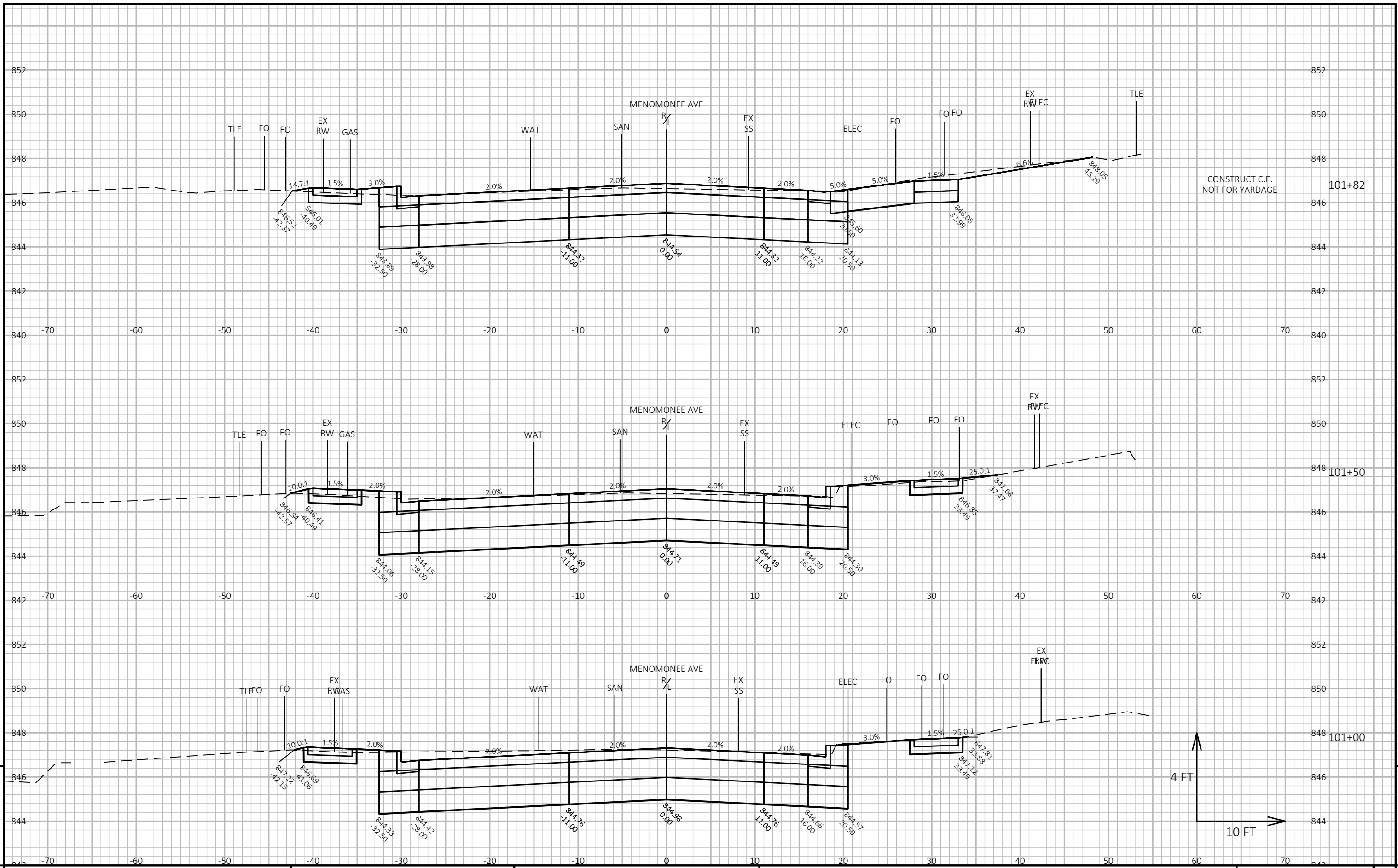
STATION	REAL STATION	DISTANCE	AREA (SF)		INCREMENTAL VOL (CY) (UNADJUSTED)		CUMULATIVE VOL (CY)		
			CUT	FILL	CUT	FILL	CUT	EXPANDED FILL	MASS ORDINATE
							1.00	1.25	
121+50	12150.00	50.00	161.34	0.00	280	0	9,732	0	9,732
122+00	12200.00	50.00	184.45	0.00	320	0	10,052	0	10,052
122+50	12250.00	50.00	164.80	0.00	323	0	10,375	0	10,375
123+00	12300.00	50.00	133.06	0.00	276	0	10,651	0	10,651
123+46	12346.46	46.46	186.33	0.00	275	0	10,926	0	10,926
123+50	12350.00	3.54	185.46	0.00	24	0	10,950	0	10,950
124+00	12400.00	50.00	113.63	0.00	277	0	11,227	0	11,227
124+50	12450.00	50.00	118.51	0.00	215	0	11,442	0	11,442
125+00	12500.00	50.00	130.79	0.00	231	0	11,673	0	11,673
125+50	12550.00	0.00	153.21	0.01	0	0	11,673	0	11,673
126+00	12600.00	50.00	153.33	0.09	284	0	11,957	0	11,957
126+47	12647.12	47.12	189.17	0.00	299	0	12,256	0	12,256
126+50	12650.00	2.88	184.59	0.00	20	0	12,276	0	12,276
127+00	12700.00	50.00	130.18	0.34	291	0	12,567	0	12,567
127+50	12750.00	50.00	132.31	0.22	243	1	12,810	1	12,809
128+00	12800.00	50.00	136.44	0.28	249	0	13,059	1	13,058
128+50	12850.00	50.00	138.41	0.82	254	1	13,313	3	13,311
129+00	12900.00	50.00	119.67	3.51	239	4	13,552	8	13,545
129+50	12950.00	50.00	178.40	0.35	276	4	13,828	13	13,816
129+63	12963.19	13.19	180.40	0.50	88	0	13,916	13	13,904
129+63	12963.19	0.00	0.00	0.00	0	0	13,916	13	13,904
130+00	13000.00	36.81	106.42	0.64	73	0	13,989	13	13,977
130+50	13050.00	50.00	125.64	0.40	215	1	14,204	14	14,190
131+00	13100.00	50.00	124.25	0.32	231	1	14,435	15	14,420
131+50	13150.00	50.00	128.79	0.14	234	0	14,669	15	14,654
132+00	13200.00	50.00	134.23	0.09	244	0	14,913	15	14,898
132+50	13250.00	50.00	236.93	0.58	344	1	15,257	16	15,241
132+65	13265.19	15.19	242.57	0.00	135	0	15,392	16	15,376
133+00	13300.00	34.81	139.46	0.00	246	0	15,638	16	15,622
133+50	13350.00	50.00	142.65	0.00	261	0	15,899	16	15,883
134+00	13400.00	50.00	157.39	0.00	278	0	16,177	16	16,161
134+50	13450.00	50.00	143.88	0.07	279	0	16,456	16	16,440
135+00	13500.00	50.00	137.04	0.00	260	0	16,716	16	16,700
135+50	13550.00	50.00	198.99	0.06	311	0	17,027	16	17,011
135+67	13566.75	16.75	200.37	1.02	124	0	17,151	16	17,135
136+00	13600.00	33.25	125.89	1.00	201	1	17,352	18	17,335
136+50	13650.00	50.00	137.49	0.29	244	1	17,596	19	17,577
137+00	13700.00	50.00	137.32	0.49	254	1	17,850	20	17,830
137+50	13750.00	50.00	129.96	0.89	247	1	18,097	21	18,076
138+00	13800.00	50.00	128.59	1.48	239	2	18,336	24	18,312
138+50	13850.00	50.00	123.65	0.08	234	1	18,570	25	18,545
138+77	13877.13	27.13	196.81	0.00	161	0	18,731	25	18,706
139+00	13900.00	22.87	142.41	0.00	144	0	18,875	25	18,850
139+50	13950.00	50.00	125.00	3.45	248	3	19,123	29	19,094
140+00	14000.00	50.00	132.57	1.23	238	4	19,361	34	19,327
140+50	14050.00	50.00	138.68	0.04	251	1	19,612	35	19,577
141+00	14100.00	50.00	125.34	1.86	244	2	19,856	38	19,819
141+50	14150.00	50.00	115.57	0.00	223	2	20,079	40	20,039
141+82	14182.10	32.10	189.09	0.00	181	0	20,260	40	20,220
142+00	14200.00	17.90	188.97	0.00	125	0	20,385	40	20,345

STATION	REAL STATION	DISTANCE	AREA (SF)		INCREMENTAL VOL (CY) (UNADJUSTED)		CUMULATIVE VOL (CY)		
			CUT	FILL	CUT	FILL	CUT	EXPANDED FILL	MASS ORDINATE
							1.00	1.25	
142+50	14250.00	50.00	137.03	0.07	302	0	20,687	40	20,647
143+00	14300.00	50.00	134.80	0.21	252	0	20,939	40	20,899
143+50	14350.00	50.00	121.83	2.00	238	2	21,177	43	21,135
144+00	14400.00	50.00	125.01	0.29	229	2	21,406	45	21,361
144+50	14450.00	50.00	127.18	0.43	234	1	21,640	46	21,594
144+91	14491.34	41.34	177.06	0.30	233	1	21,873	48	21,826
145+00	14500.00	8.66	178.09	0.24	57	0	21,930	48	21,883
145+50	14550.00	50.00	128.57	0.43	284	1	22,214	49	22,165
146+00	14600.00	50.00	135.24	0.37	244	1	22,458	50	22,408
146+50	14650.00	50.00	124.64	0.60	241	1	22,699	51	22,648
146+91	14691.40	0.00	131.44	0.26	0	0	22,699	51	22,648
147+00	14700.00	8.60	131.62	0.21	42	0	22,741	51	22,690
147+50	14750.00	50.00	122.28	0.75	235	1	22,976	53	22,924
148+00	14800.00	50.00	235.59	0.00	331	1	23,307	54	23,253
148+07	14806.97	6.97	235.16	0.00	61	0	23,368	54	23,314
148+50	14850.00	43.03	109.32	3.91	275	3	23,643	58	23,586
149+00	14900.00	50.00	128.28	0.43	220	4	23,863	63	23,801
149+50	14950.00	50.00	139.64	0.00	248	0	24,111	63	24,049
149+55	14954.67	4.67	139.59	0.00	24	0	24,135	63	24,073
150+00	15000.00	45.33	127.83	0.47	224	0	24,359	63	24,297
150+50	15050.00	50.00	125.09	0.98	234	1	24,593	64	24,529
150+74	15073.59	23.59	129.60	0.34	111	1	24,704	65	24,639
151+00	15100.00	26.41	130.20	0.12	127	0	24,831	65	24,766
151+26	15126.47	26.47	187.35	0.32	156	0	24,987	65	24,922
151+50	15150.00	23.53	132.55	2.15	139	1	25,126	66	25,060
152+00	15200.00	50.00	115.69	1.25	230	3	25,356	70	25,286
152+50	15250.00	50.00	116.81	0.36	215	1	25,571	71	25,500
152+66	15265.86	15.86	127.22	0.00	72	0	25,643	71	25,572
152+73	15272.57	6.71	126.69	0.00	32	0	25,675	71	25,604
153+00	15300.00	27.43	120.03	0.37	125	0	25,800	71	25,729
153+22	15322.23	22.23	114.21	0.00	96	0	25,896	71	25,825
153+50	15350.00	27.77	104.73	1.05	113	1	26,009	73	25,937
154+00	15400.00	50.00	108.71	1.25	198	2	26,207	75	26,132
154+47	15447.05	47.05	178.05	0.89	250	2	26,457	78	26,380
154+50	15450.00	2.95	178.75	0.99	19	0	26,476	78	26,399
154+55	15455.48	5.48	180.11	1.25	36	0	26,512	78	26,435
155+00	15500.00	44.52	111.32	1.77	240	2	26,752	80	26,672
155+50	15550.00	50.00	101.53	0.54	197	2	26,949	83	26,867
156+00	15600.00	50.00	92.49	4.27	180	4	27,129	88	27,042
156+11	15611.08	11.08	97.16	1.56	39	1	27,168	89	27,079
156+42	15642.26	31.18	97.71	2.10	113	2	27,281	91	27,190
156+50	15650.00	7.74	97.46	2.12	28	1	27,309	93	27,217
157+00	15700.00	50.00	97.00	3.12	180	5	27,489	99	27,390
157+21	15721.11	21.11	97.74	3.30	76	3	27,565	103	27,463
157+50	15750.00	28.89	115.06	3.71	114	4	27,679	108	27,572

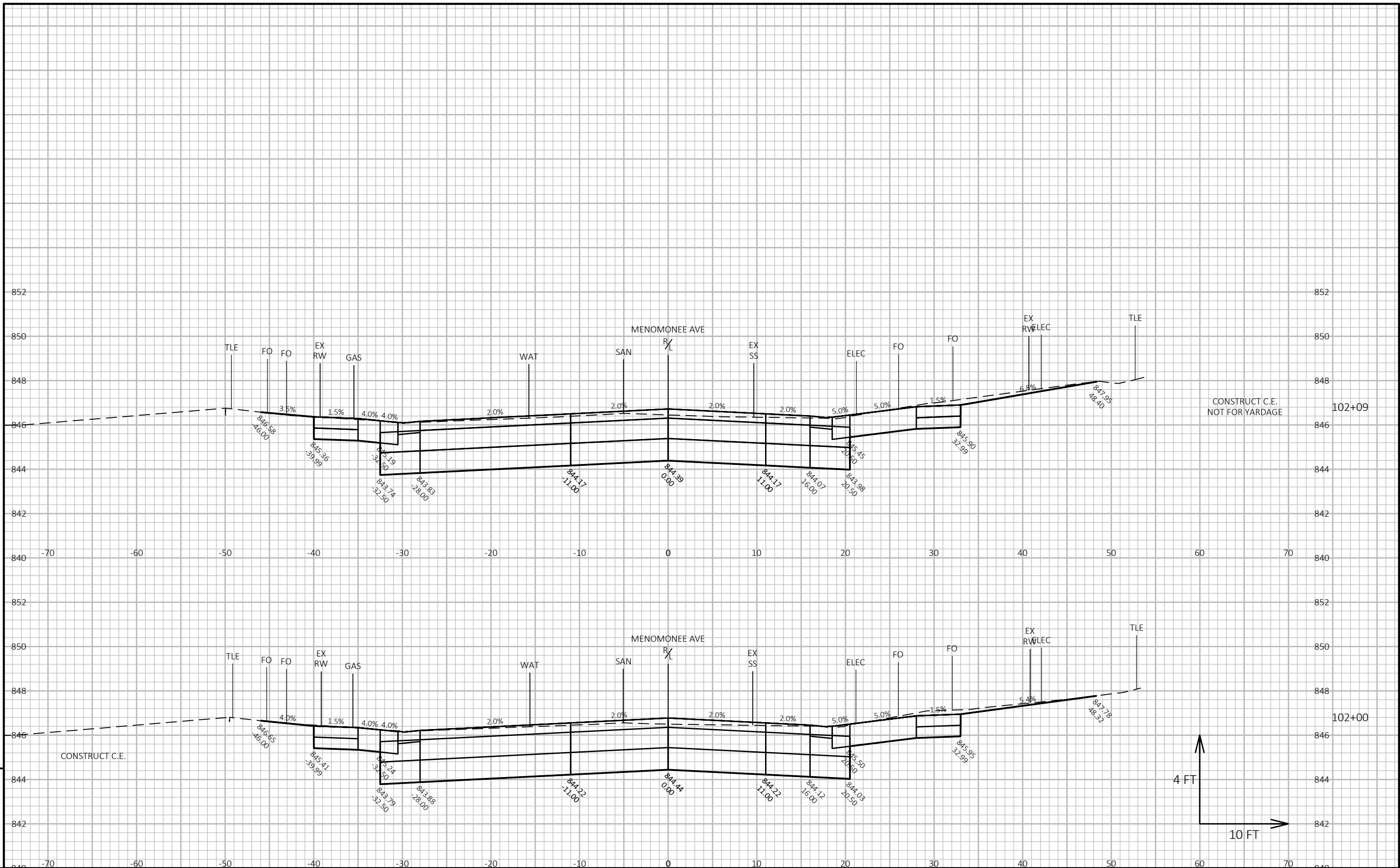
STATION	REAL STATION	DISTANCE	AREA (SF)		INCREMENTAL VOL (CY) (UNADJUSTED)		CUMULATIVE VOL (CY)		
			CUT	FILL	CUT	FILL	CUT	EXPANDED FILL	MASS ORDINATE
							1.00	1.25	
157+71	15771.19	21.19	178.35	2.62	115	2	27,794	110	27,684
157+92	15791.77	20.58	138.10	0.96	121	1	27,915	111	27,804
158+00	15800.00	8.23	110.20	0.26	38	0	27,953	111	27,842
158+03	15802.80	2.80	107.65	0.04	11	0	27,964	111	27,853
158+50	15850.00	47.20	119.69	0.68	199	1	28,163	113	28,051
159+00	15900.00	50.00	127.90	0.13	229	1	28,392	114	28,278
159+50	15950.00	50.00	200.92	0.27	304	0	28,696	114	28,582
159+52	15952.37	2.37	200.72	0.39	18	0	28,714	114	28,600
159+79	15979.19	26.82	119.00	0.00	159	0	28,873	114	28,759
160+00	16000.00	20.81	114.43	0.92	90	0	28,963	114	28,849
160+50	16050.00	50.00	104.83	1.58	203	2	29,166	116	29,050
160+84	16084.38	34.38	114.07	0.09	139	1	29,305	118	29,188
161+00	16100.00	15.62	108.96	0.97	65	0	29,370	118	29,253
161+50	16150.00	50.00	120.60	0.00	213	1	29,583	119	29,464
162+00	16200.00	50.00	113.43	2.22	217	2	29,800	121	29,679
162+24	16223.72	23.72	120.15	0.78	103	1	29,903	123	29,781
162+50	16250.00	26.28	114.12	1.21	114	1	30,017	124	29,893
163+00	16300.00	50.00	110.52	1.21	208	2	30,225	126	30,099
163+50	16350.00	50.00	114.28	1.81	208	3	30,433	130	30,303
163+74	16374.36	24.36	125.32	0.16	108	1	30,541	131	30,410
164+00	16400.00	25.64	114.08	2.64	114	1	30,655	133	30,523
164+50	16450.00	50.00	119.51	1.47	216	4	30,871	138	30,734
165+00	16500.00	50.00	118.25	0.42	220	2	31,091	140	30,951
165+50	16550.00	50.00	121.40	0.71	222	1	31,313	141	31,172
166+00	16600.00	50.00	122.98	0.77	226	1	31,539	143	31,397
166+50	16650.00	50.00	109.26	2.54	215	3	31,754	146	31,608
166+65	16664.92	14.92	111.34	1.71	61	1	31,815	148	31,668
167+00	16700.00	35.08	106.35	5.19	141	4	31,956	153	31,804
167+41	16741.37	41.37	107.02	1.29	163	5	32,119	159	31,960
167+50	16750.00	8.63	106.93	1.20	34	0	32,153	159	31,994
167+80	16780.17	30.17	104.96	1.99	118	2	32,271	161	32,110
167+86	16786.45	6.28	102.15	1.99	24	0	32,295	161	32,134
168+00	16800.00	13.55	88.09	8.29	48	3	32,343	165	32,178
168+50	16850.00	50.00	86.79	4.83	162	12	32,505	180	32,325
169+00	16900.00	50.00	89.70	3.71	163	8	32,668	190	32,478
169+50	16950.00	50.00	91.28	3.11	168	6	32,836	198	32,639
170+00	17000.00	50.00	79.46	8.80	158	11	32,994	211	32,783
170+50	17050.00	50.00	76.58	5.87	144	14	33,138	229	32,909
171+00	17100.00	50.00	71.40	10.34	137	15	33,275	248	33,028
171+50	17150.00	50.00	68.20	7.14	129	16	33,404	268	33,137
171+92	17191.91	41.91	73.91	5.26	110	10	33,514	280	33,234
172+00	17200.00	8.09	74.55	4.63	22	1	33,536	281	33,255
172+50	17250.00	50.00	89.27	2.24	152	6	33,688	289	33,399
173+00	17300.00	50.00	116.39	0.00	190	2	33,878	291	33,587
173+25	17325.19	25.19	122.08	0.47	111	0	33,989	291	33,698
173+50	17350.00	24.81	10.83	2.16	61	1	34,050	293	33,758
173+83	17383.20	33.20	3.73	0.00	9	1	34,059	294	33,765
174+00	17400.00	16.80	0.00	0.00	1	0	34,060	294	33,766



PROJECT NO: 2720-07-71 HWY: MENOMONEE AVE COUNTY: WAUKESHA CROSS SECTIONS: MENOMONEE AVE SHEET E



PROJECT NO: 2720-07-71 HWY: MENOMONEE AVE COUNTY: WAUKESHA CROSS SECTIONS: MENOMONEE AVE SHEET E



PROJECT NO: 2720-07-71

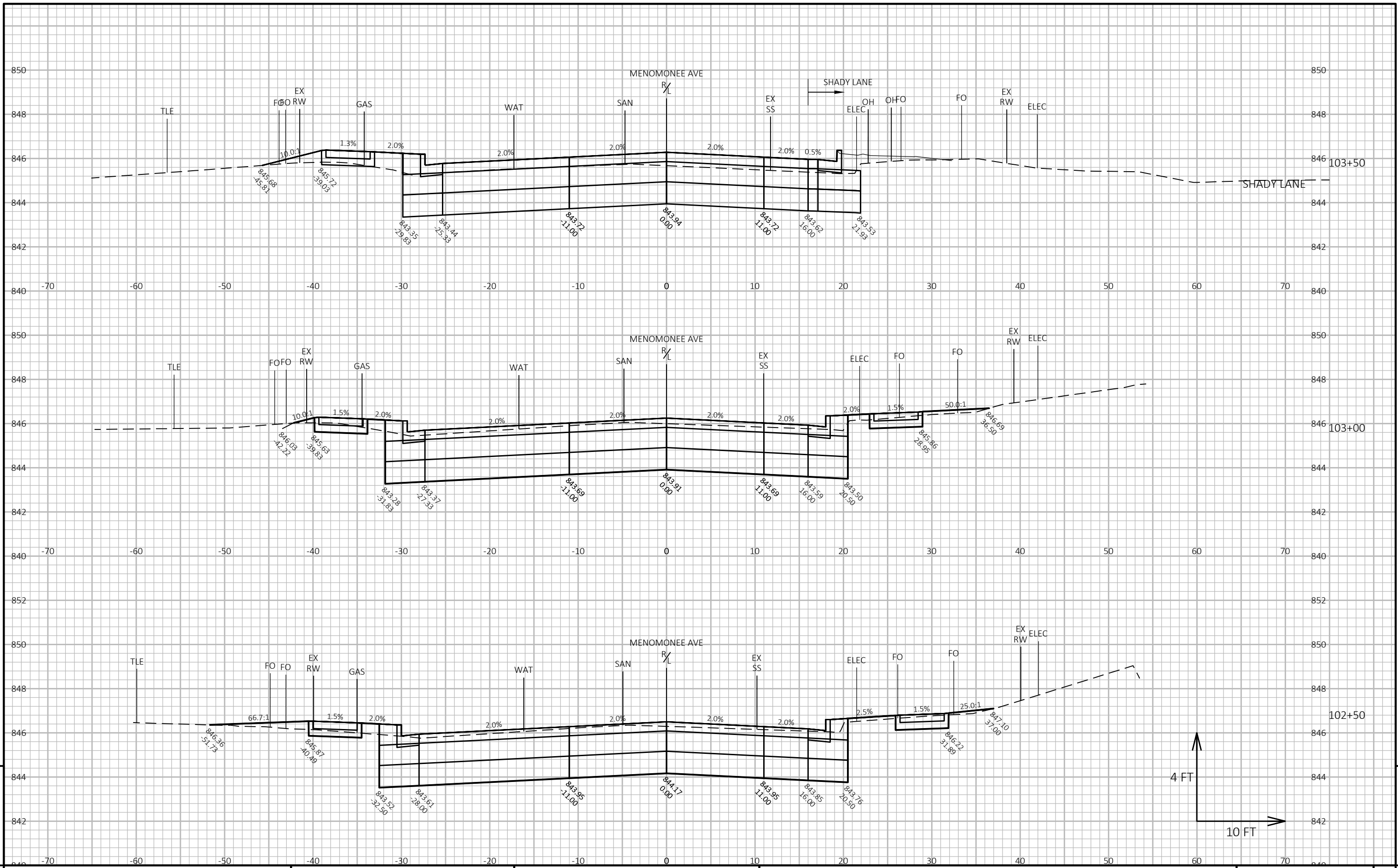
HWY: MENOMONEE AVE

COUNTY: WAUKESHA

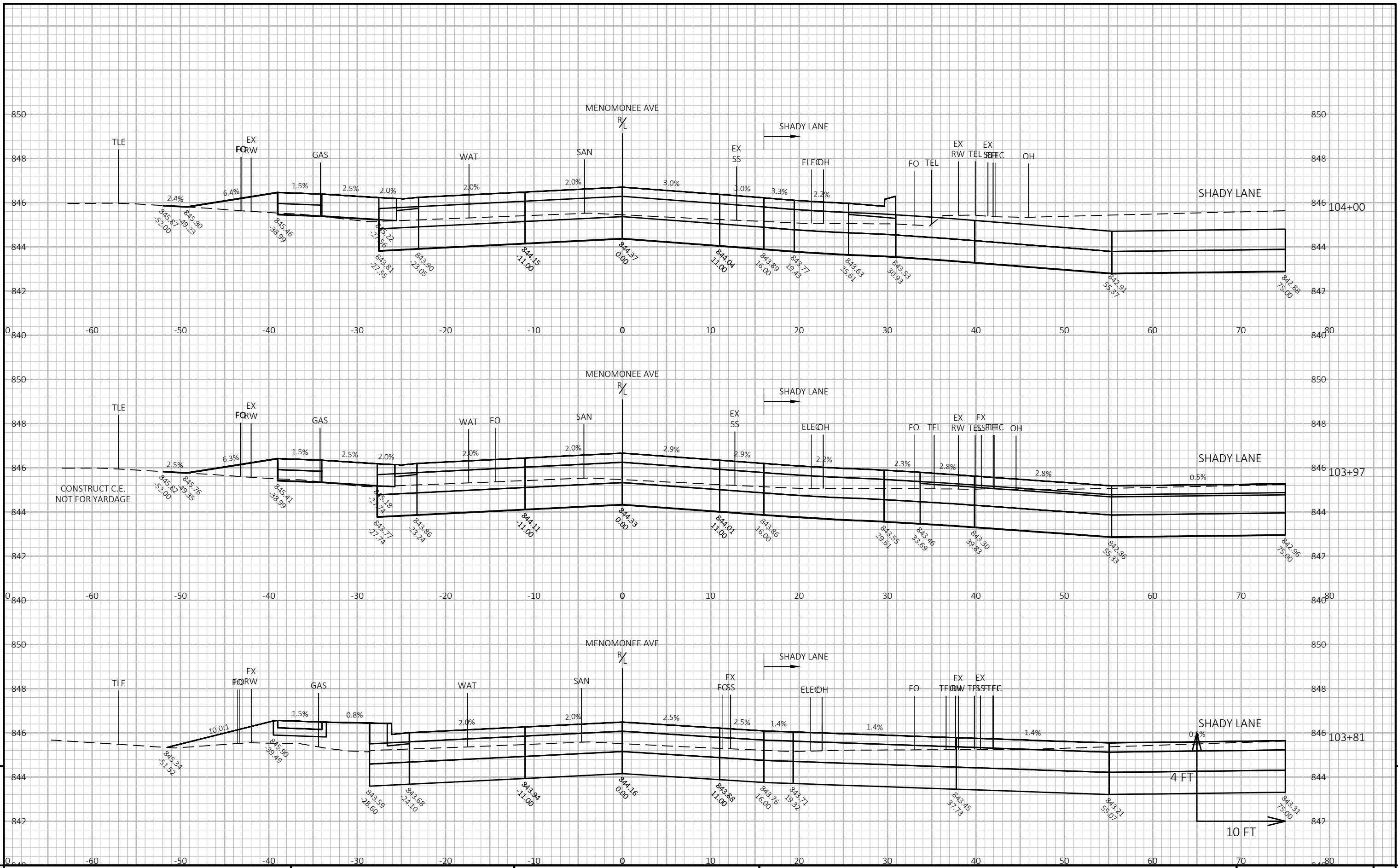
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SHEET

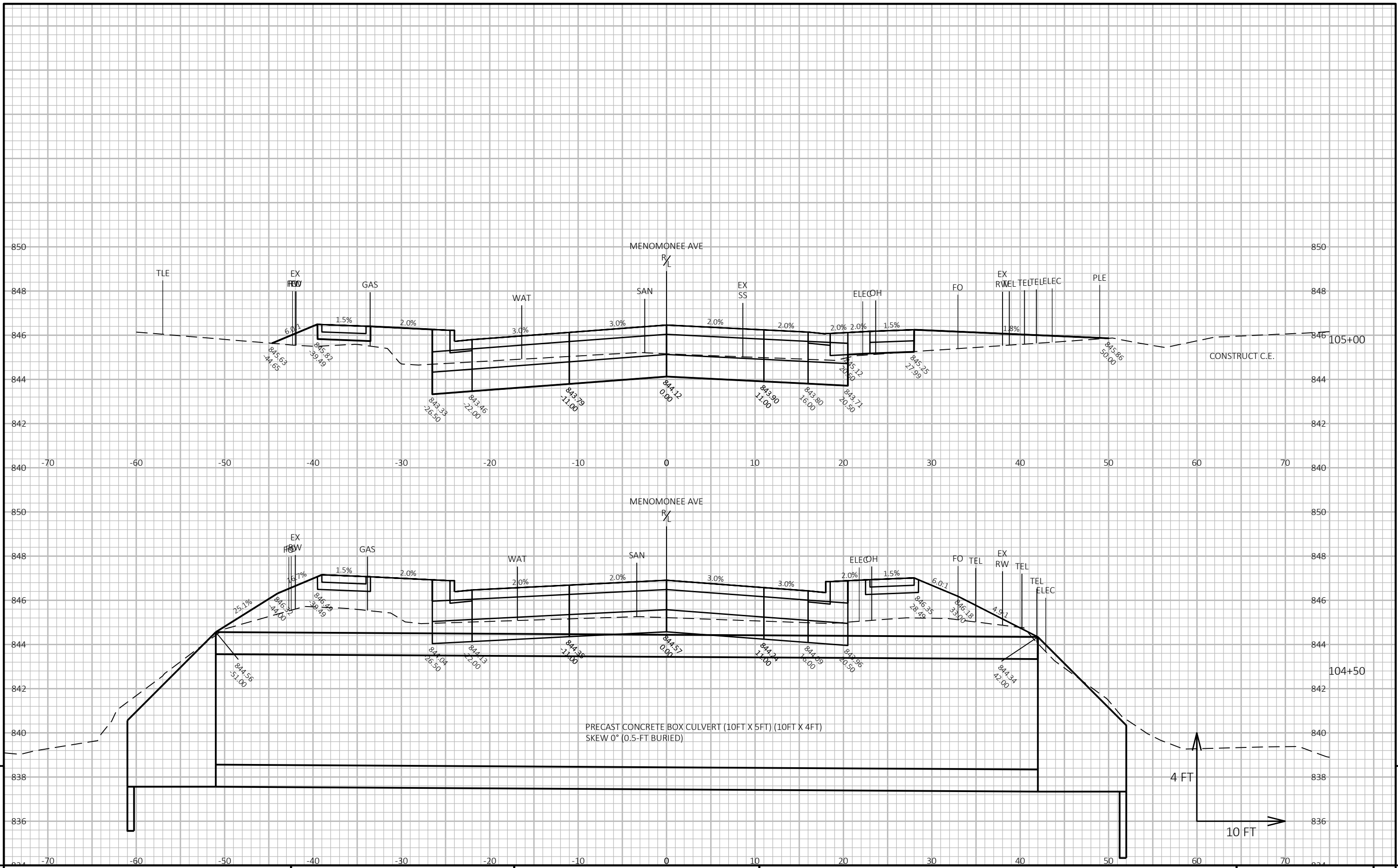
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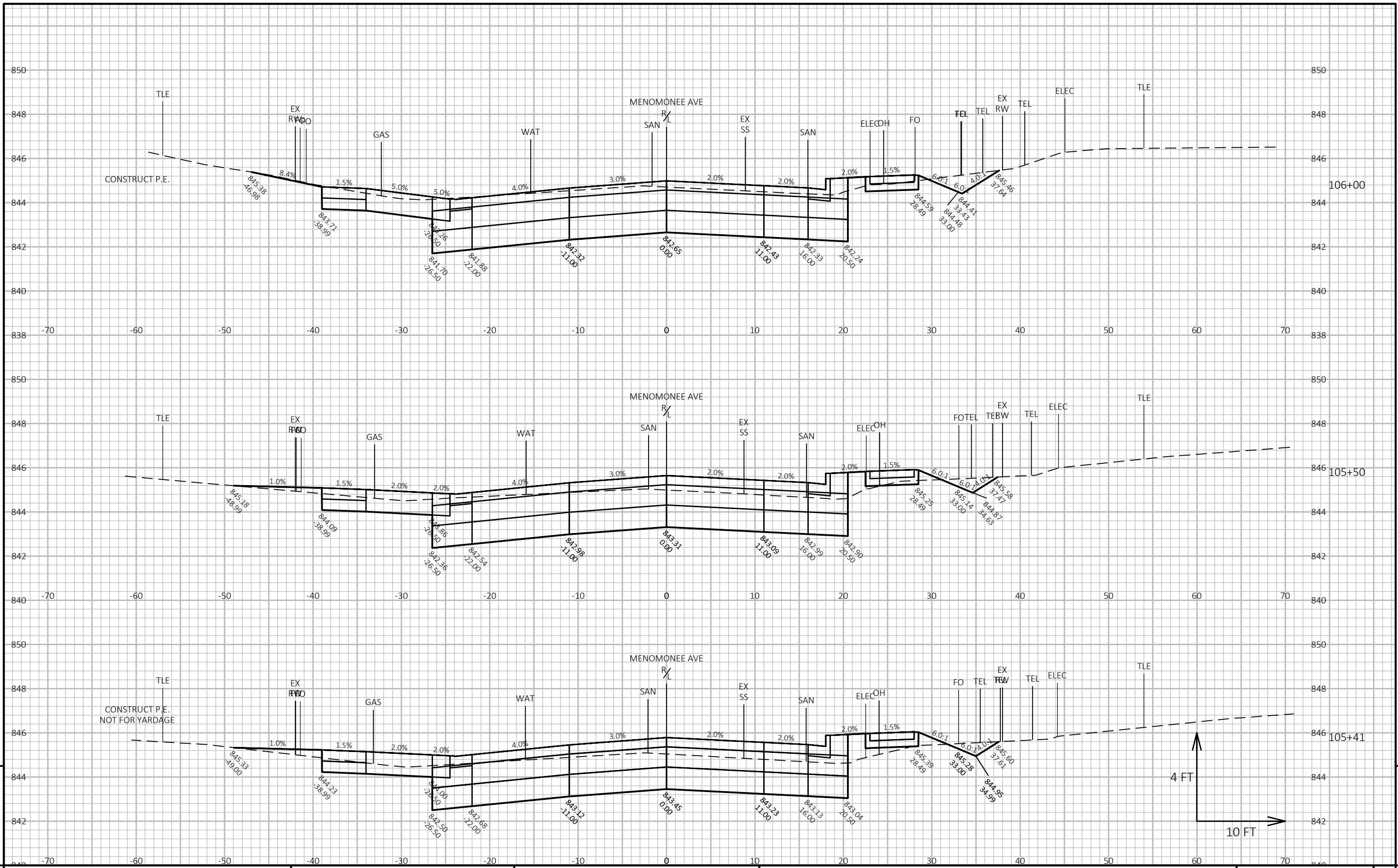
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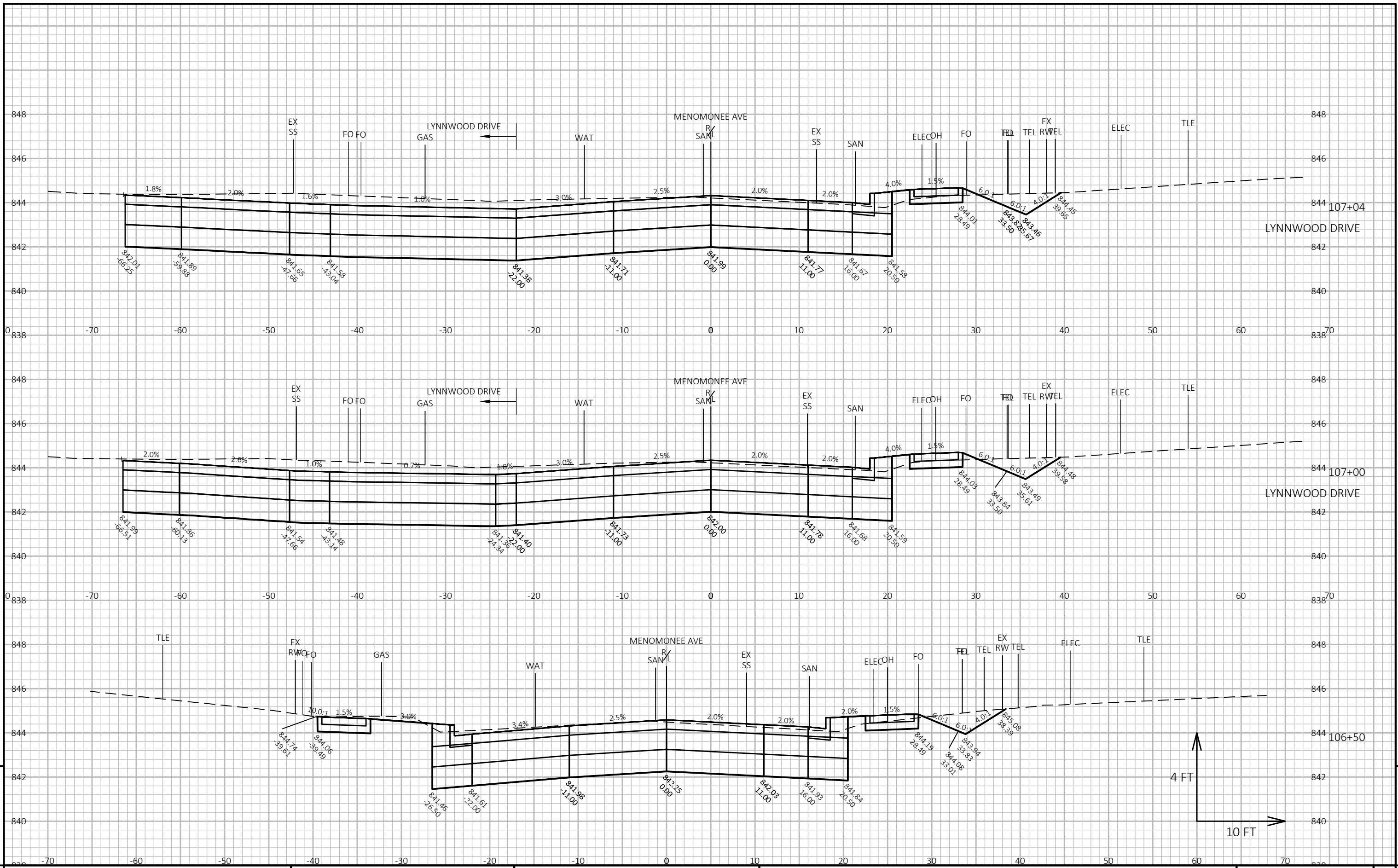
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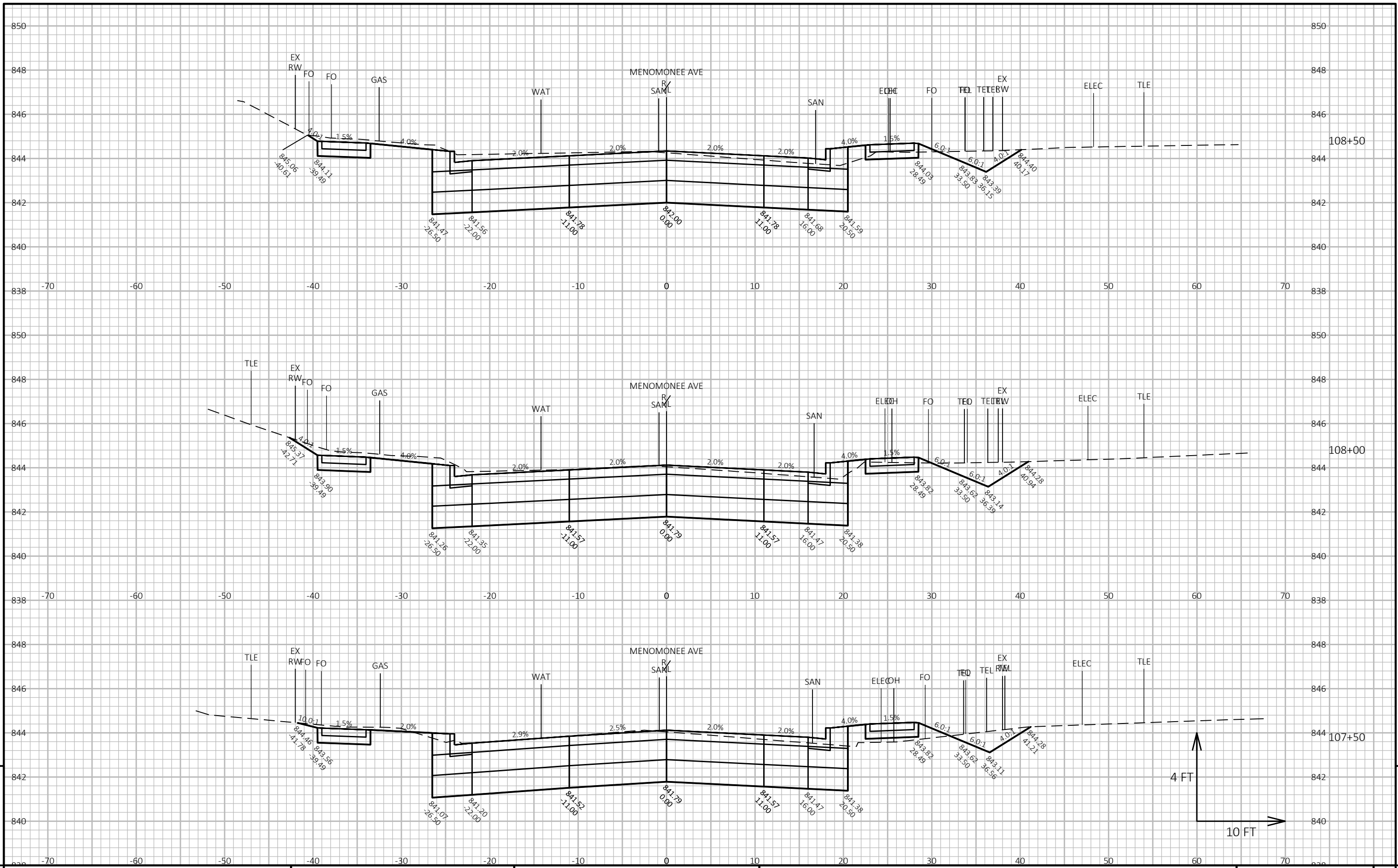
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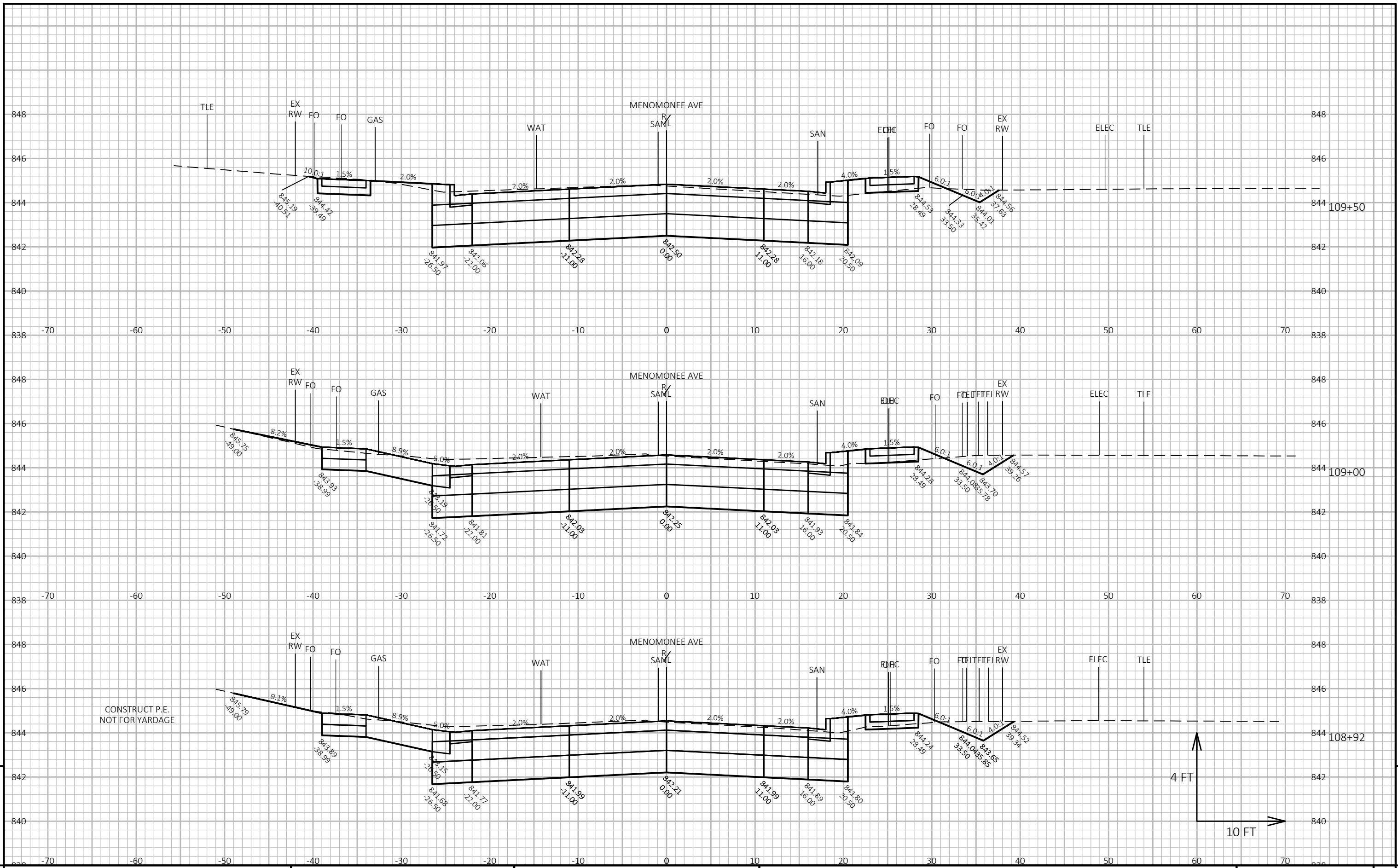
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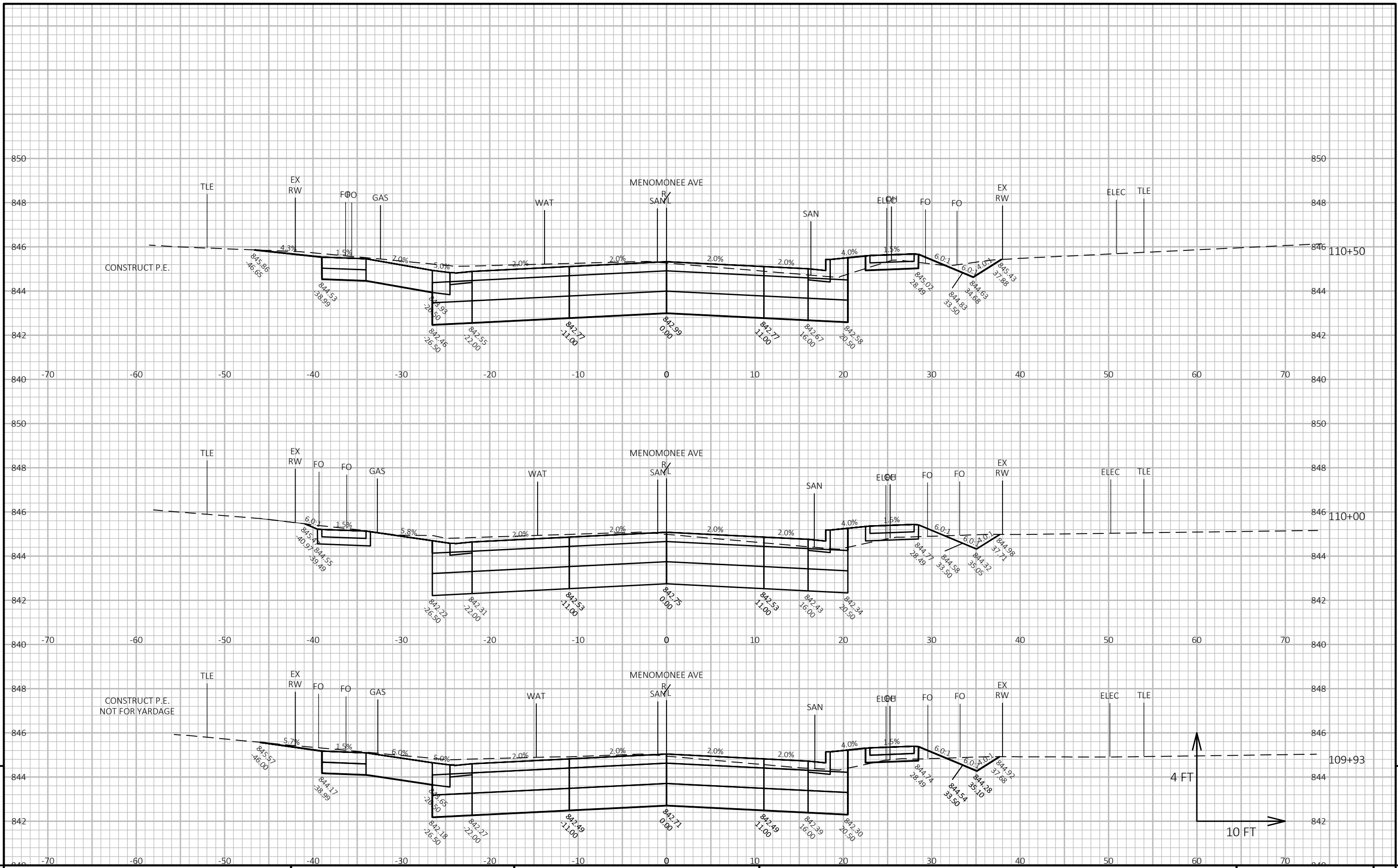
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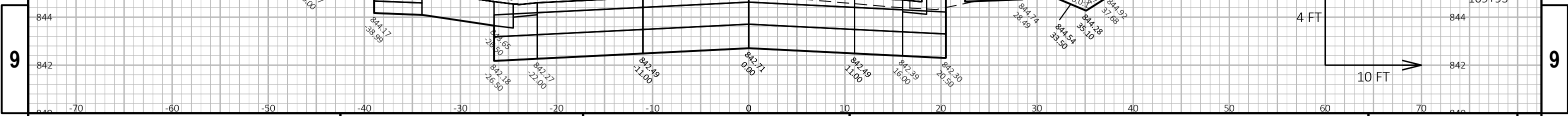
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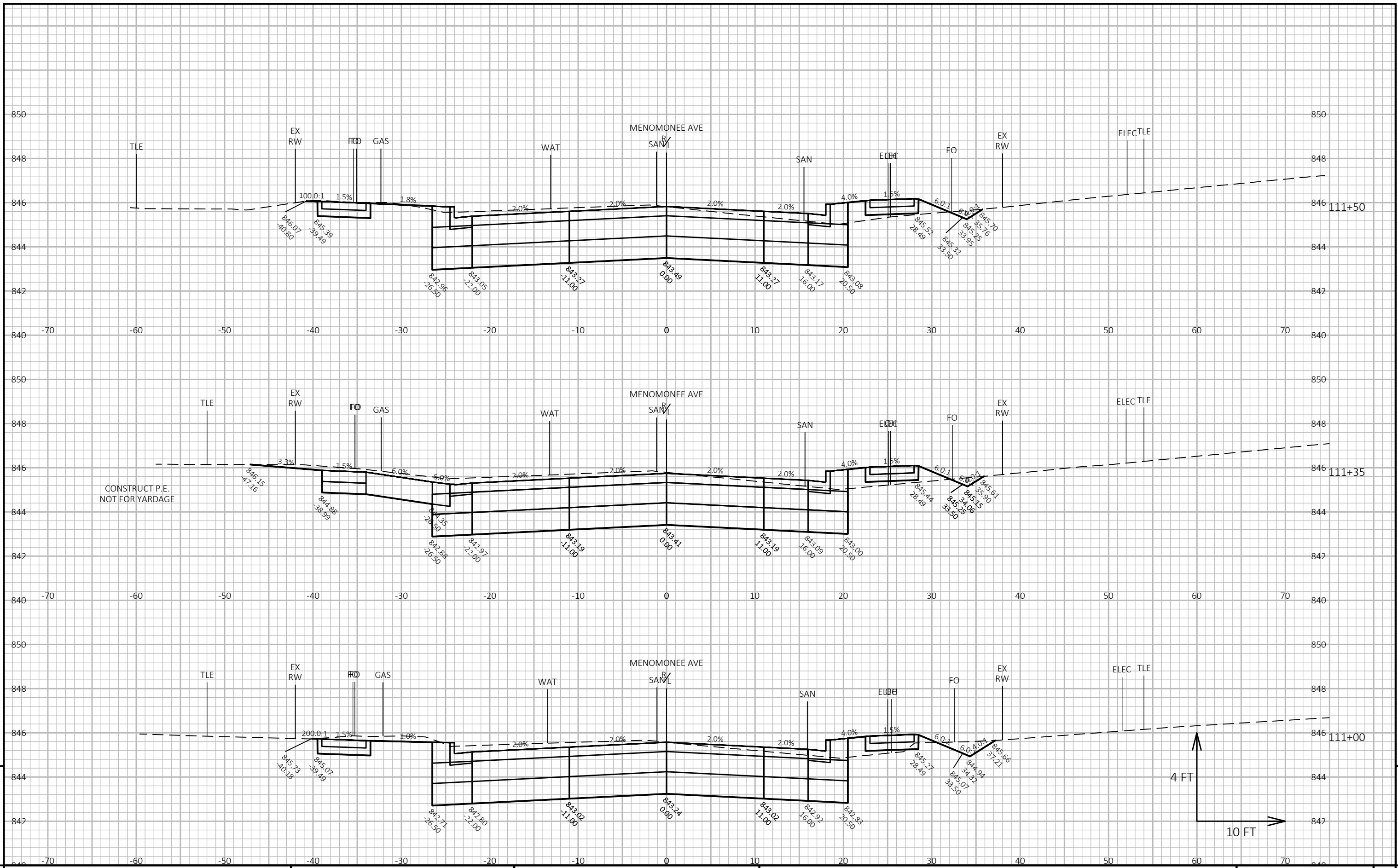


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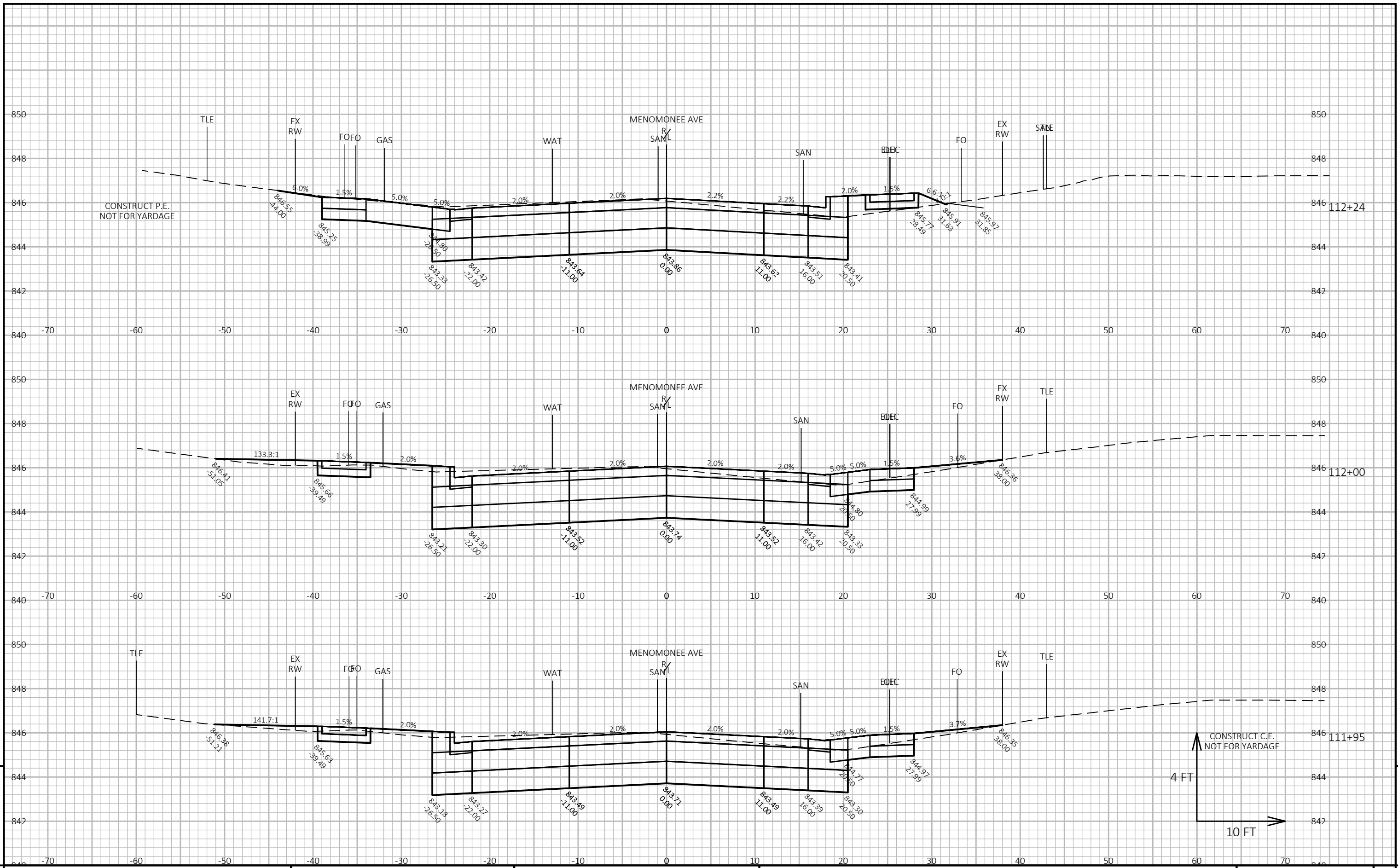


PROJECT NO: 2720-07-71	HWY: MENOMONEE AVE	COUNTY: WAUKESHA	CROSS SECTIONS: MENOMONEE AVE	SHEET
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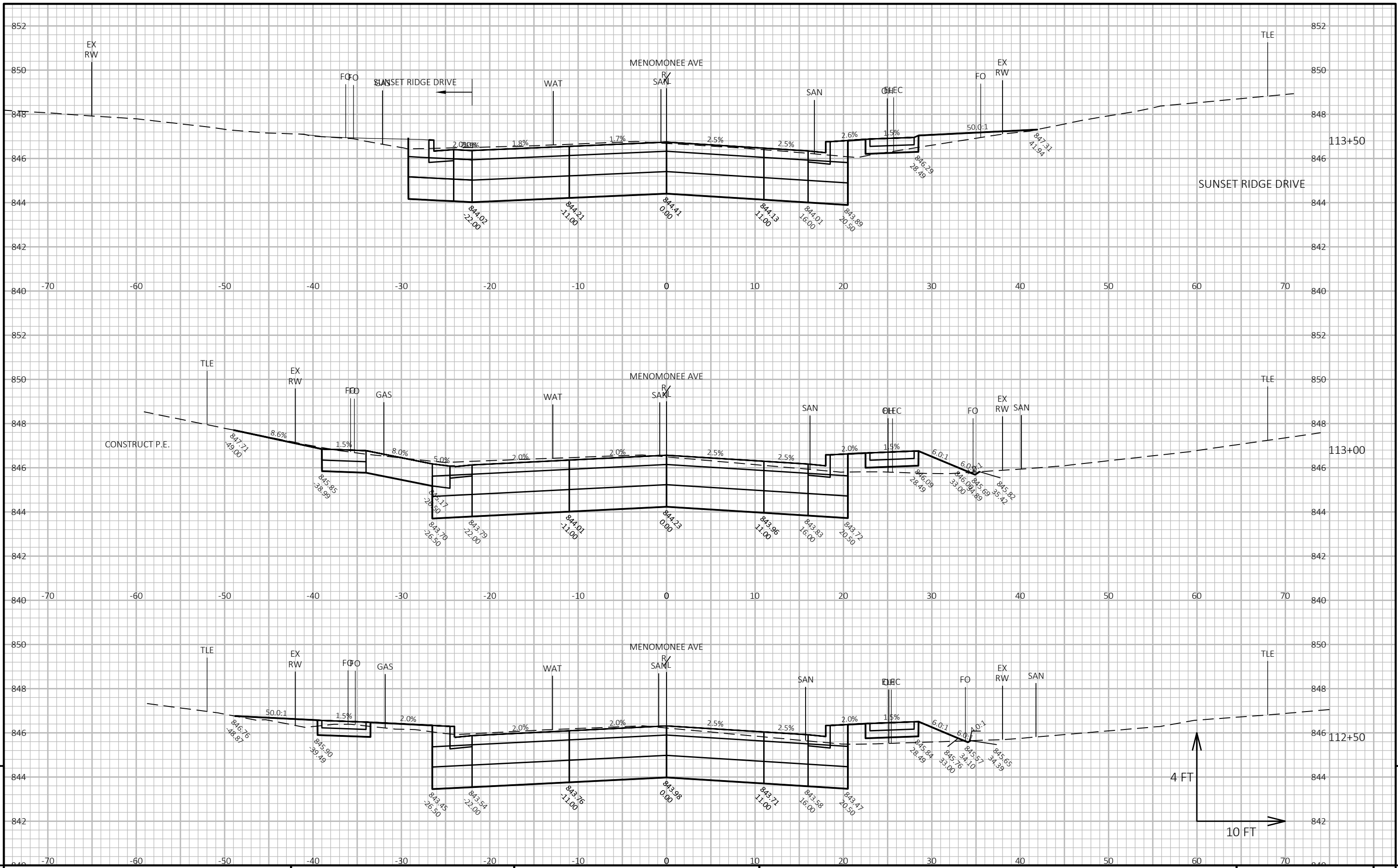




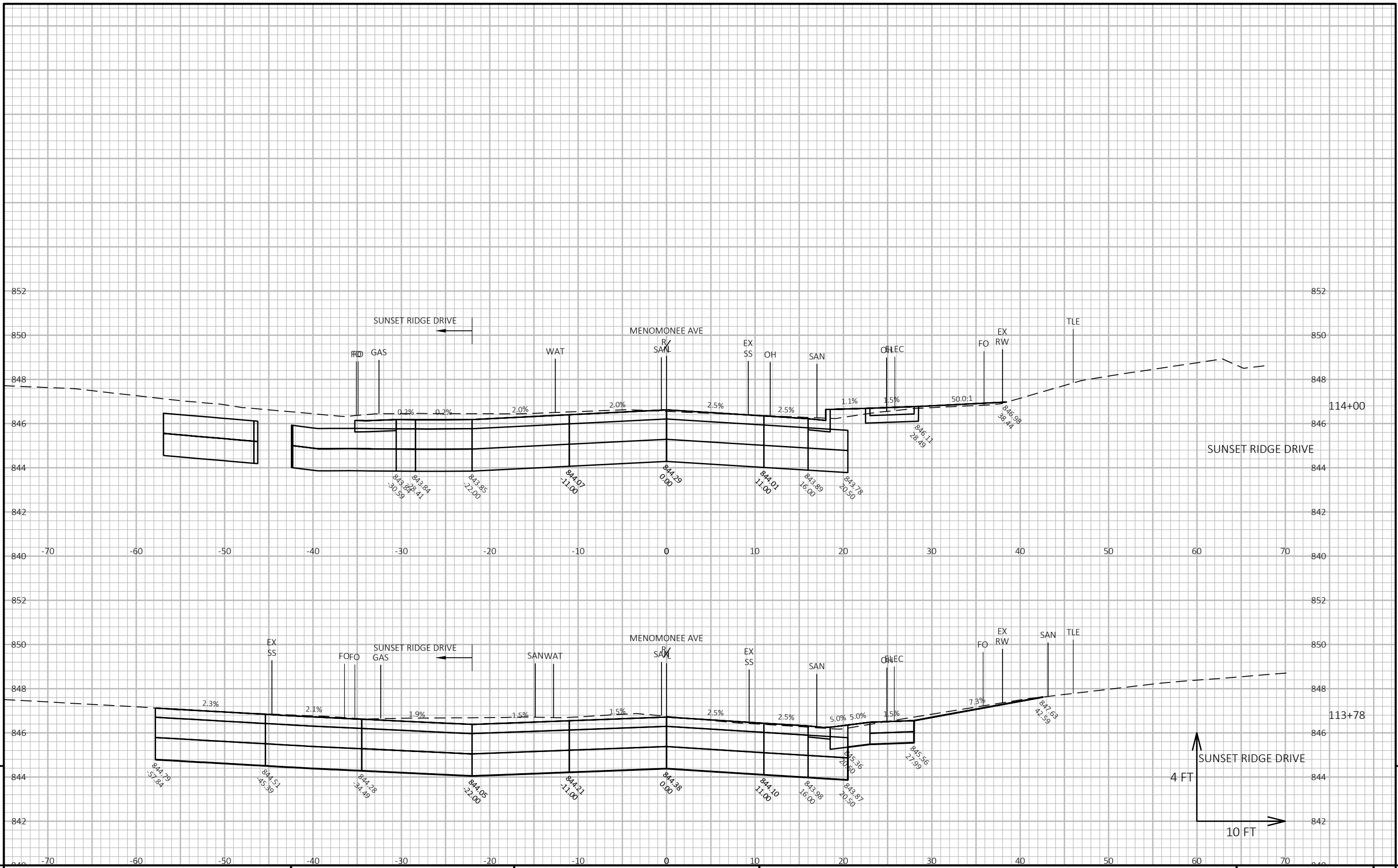
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PROJECT NO: 2720-07-71 HWY: MENOMONEE AVE COUNTY: WAUKESHA CROSS SECTIONS: MENOMONEE AVE SHEET E



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PROJECT NO: 2720-07-71

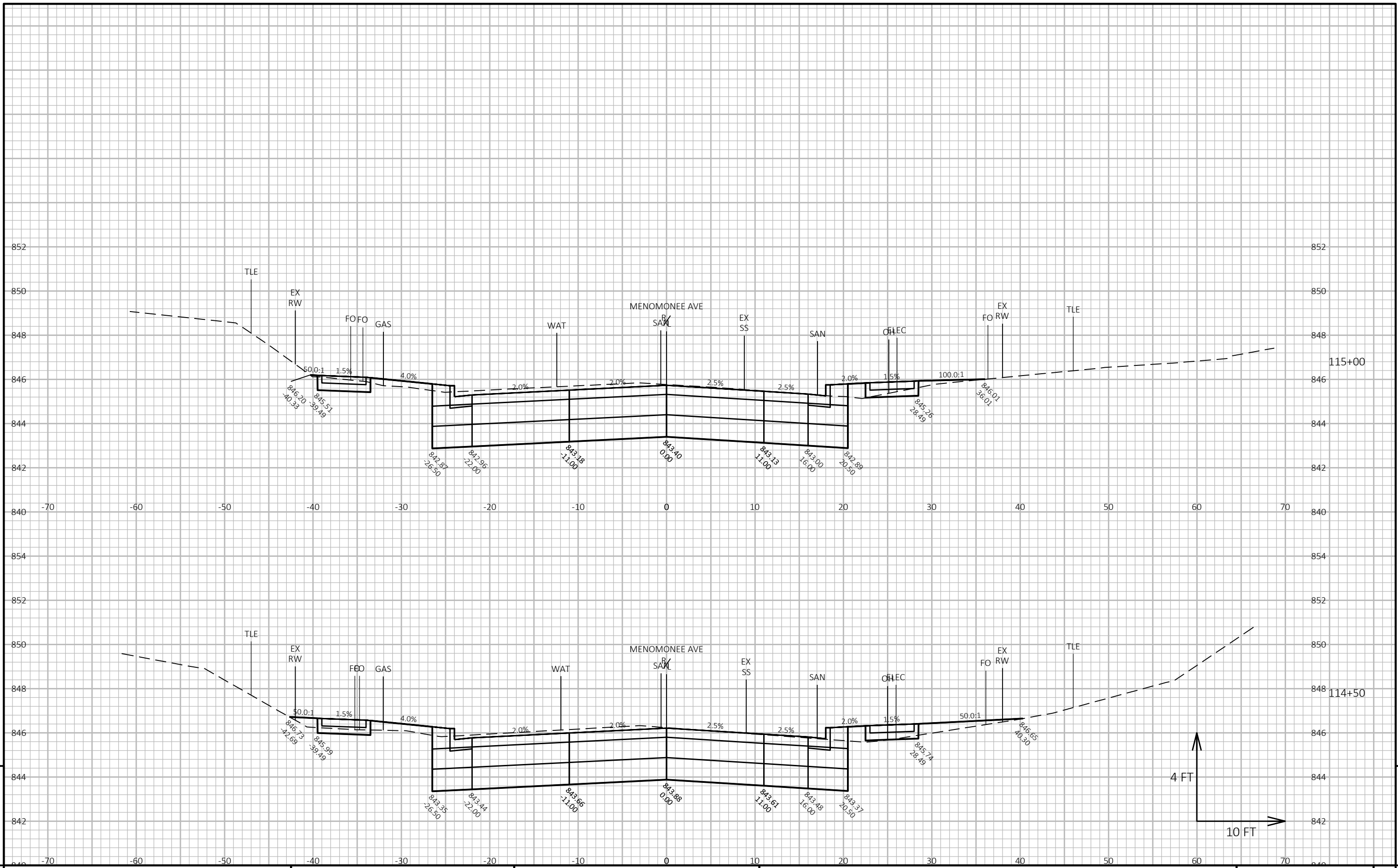
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COUNTY: WAUKESHA

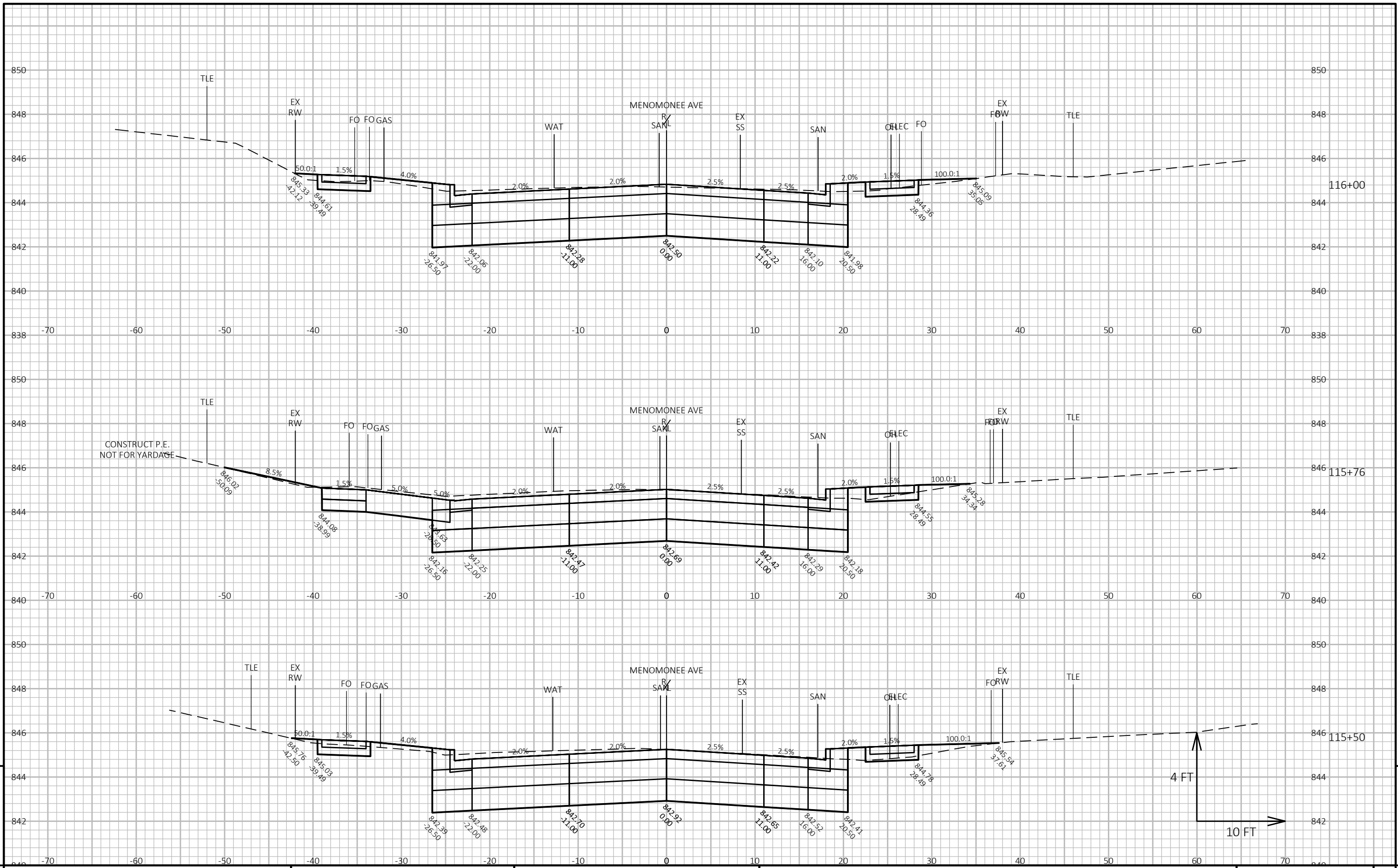
CROSS SECTIONS: MENOMONEE AVE

SHEET

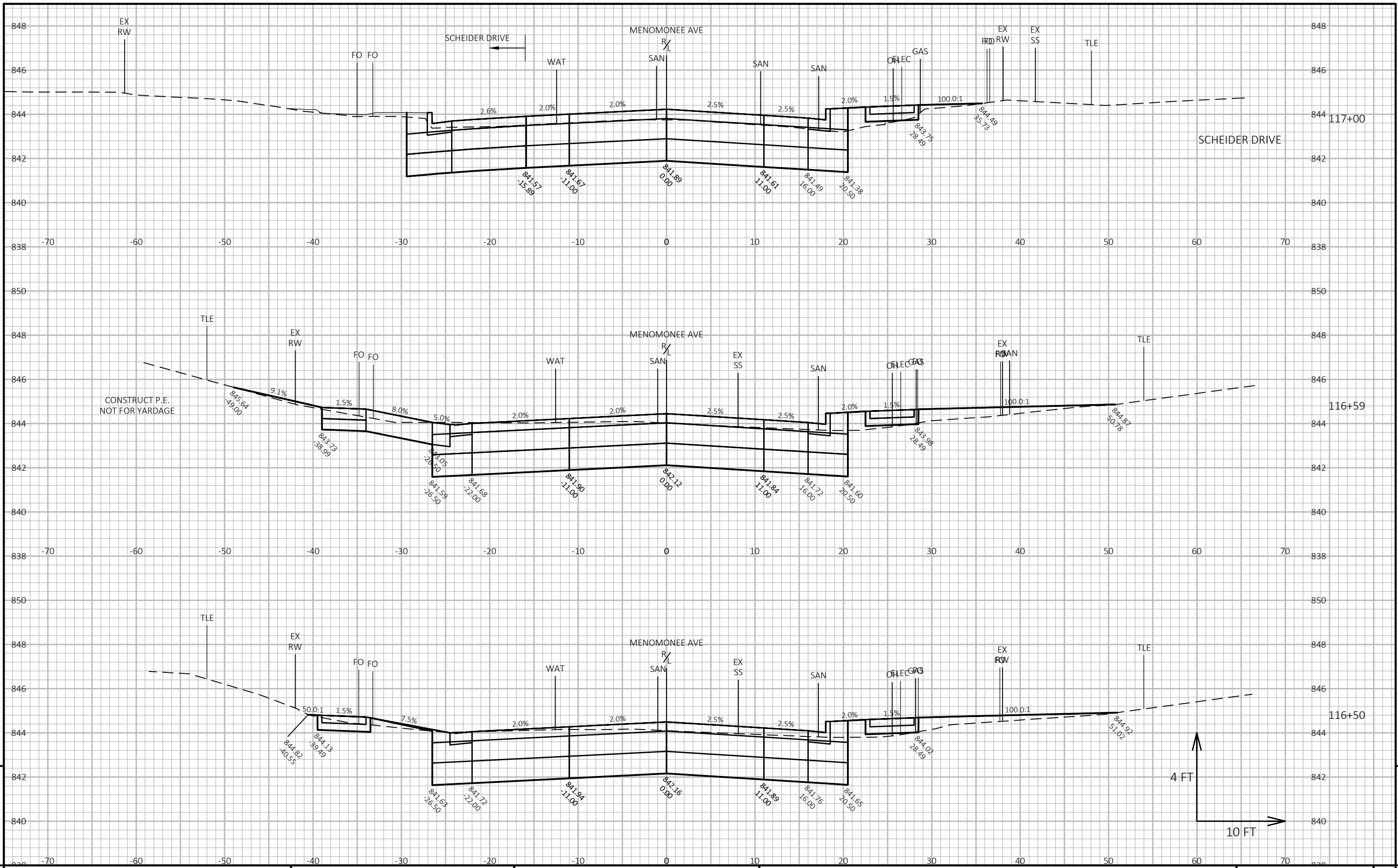
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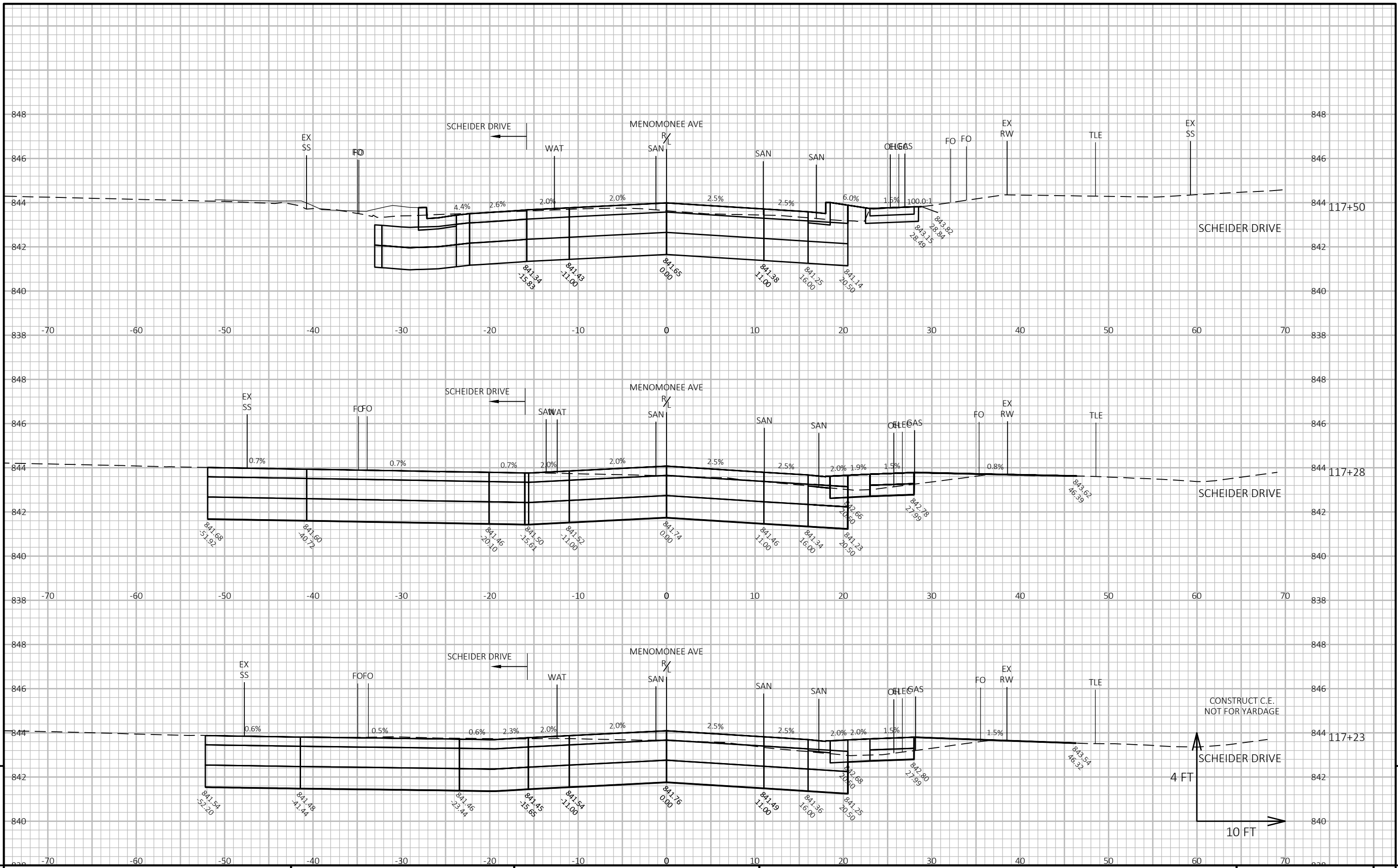
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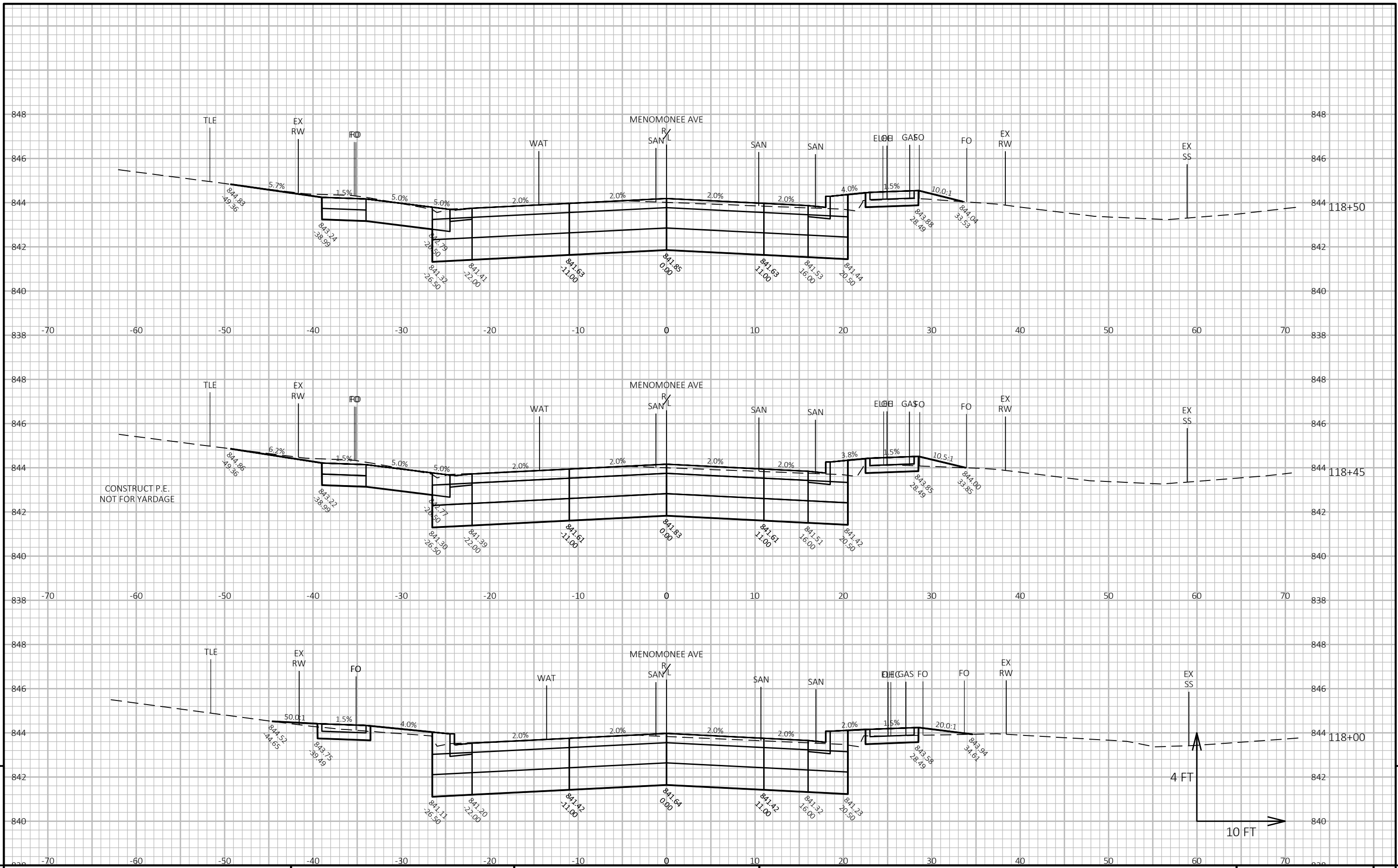
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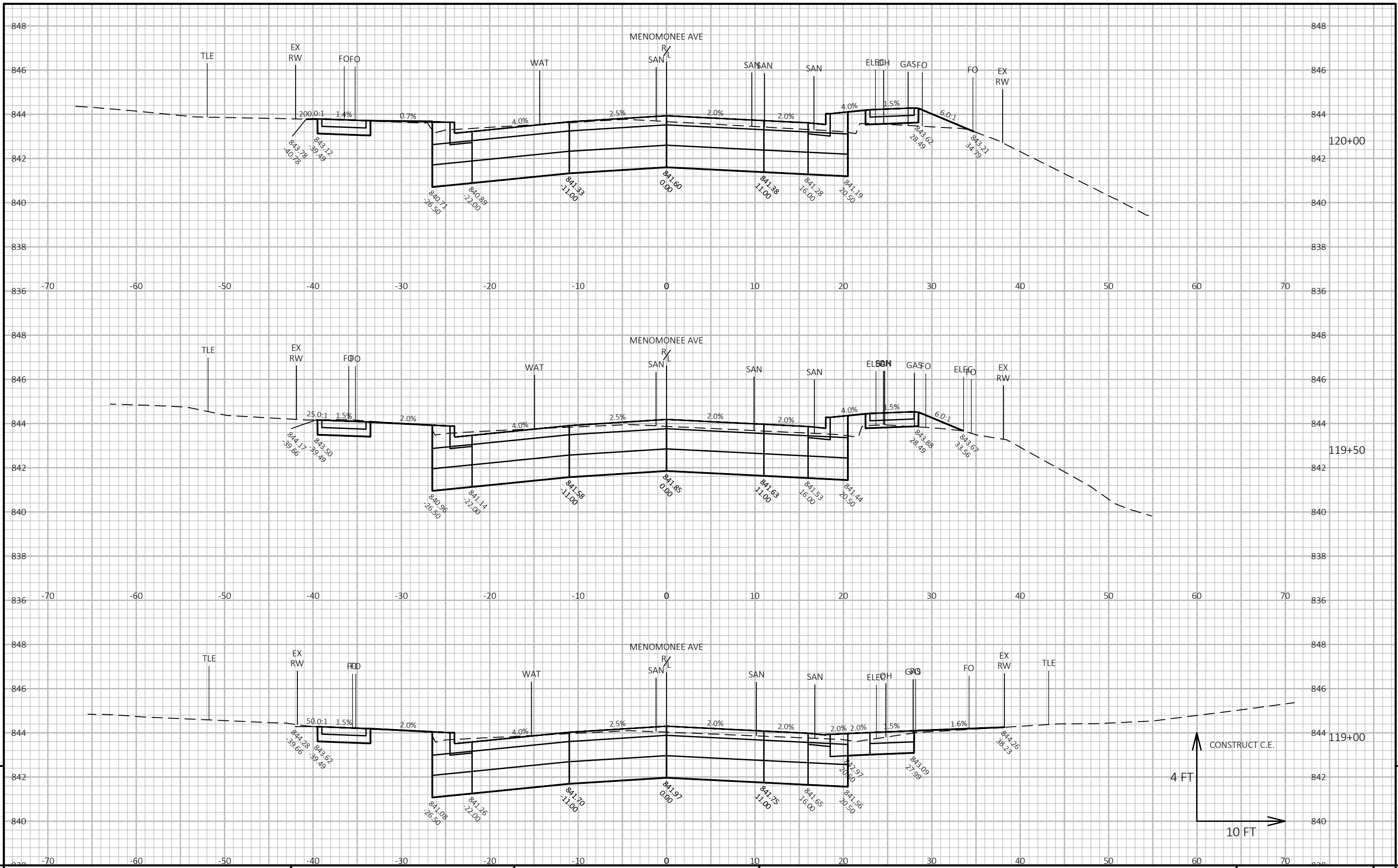
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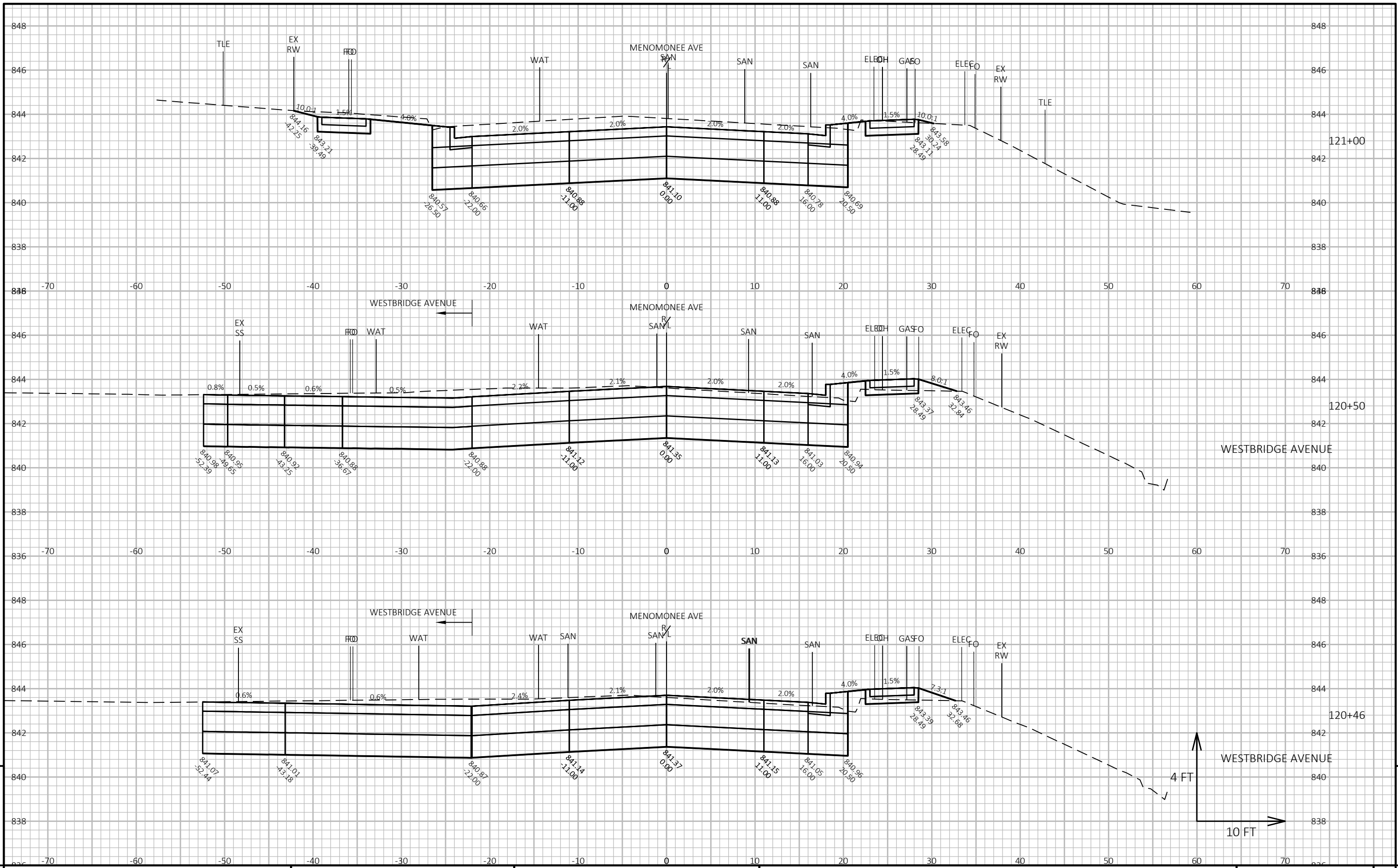
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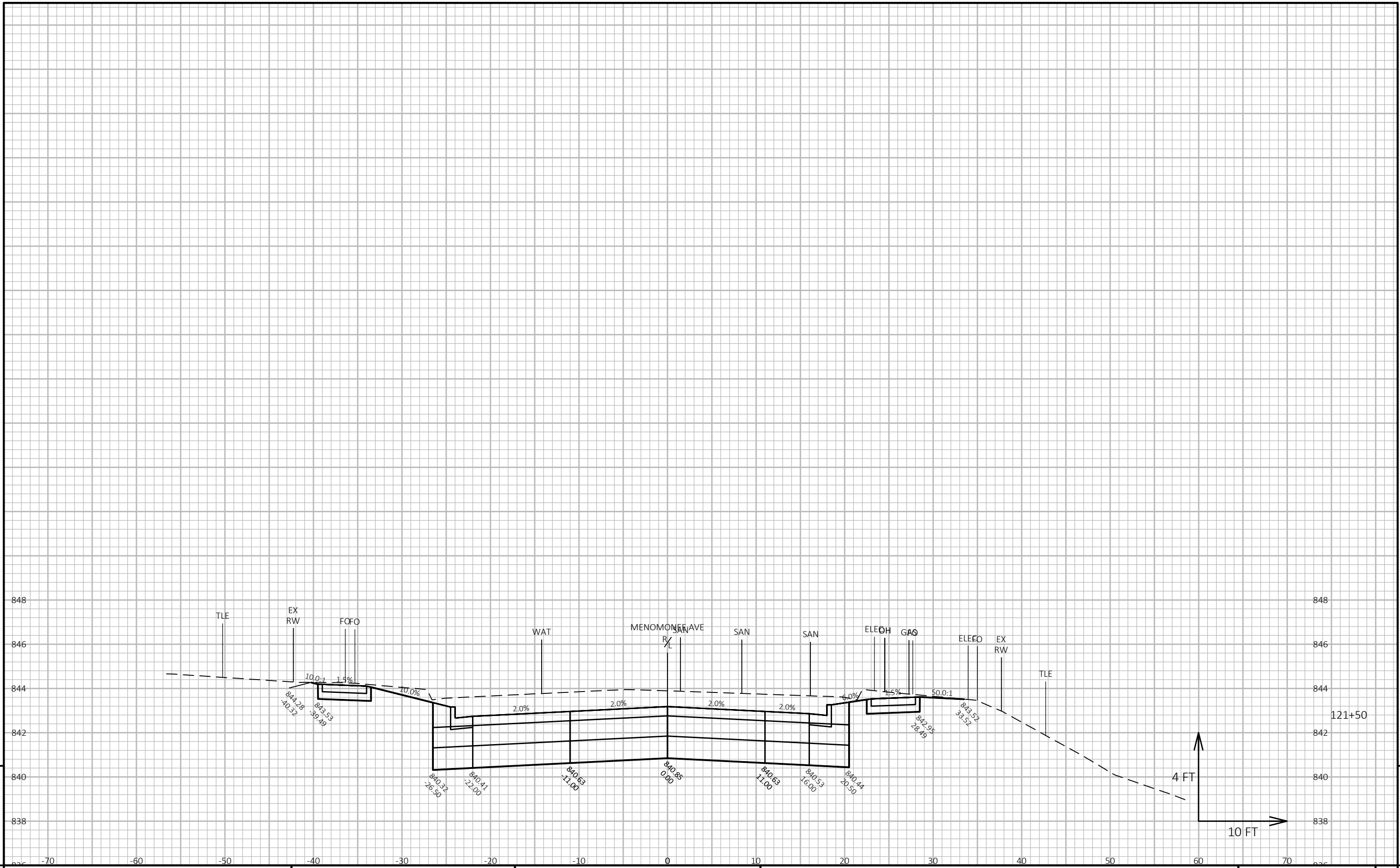
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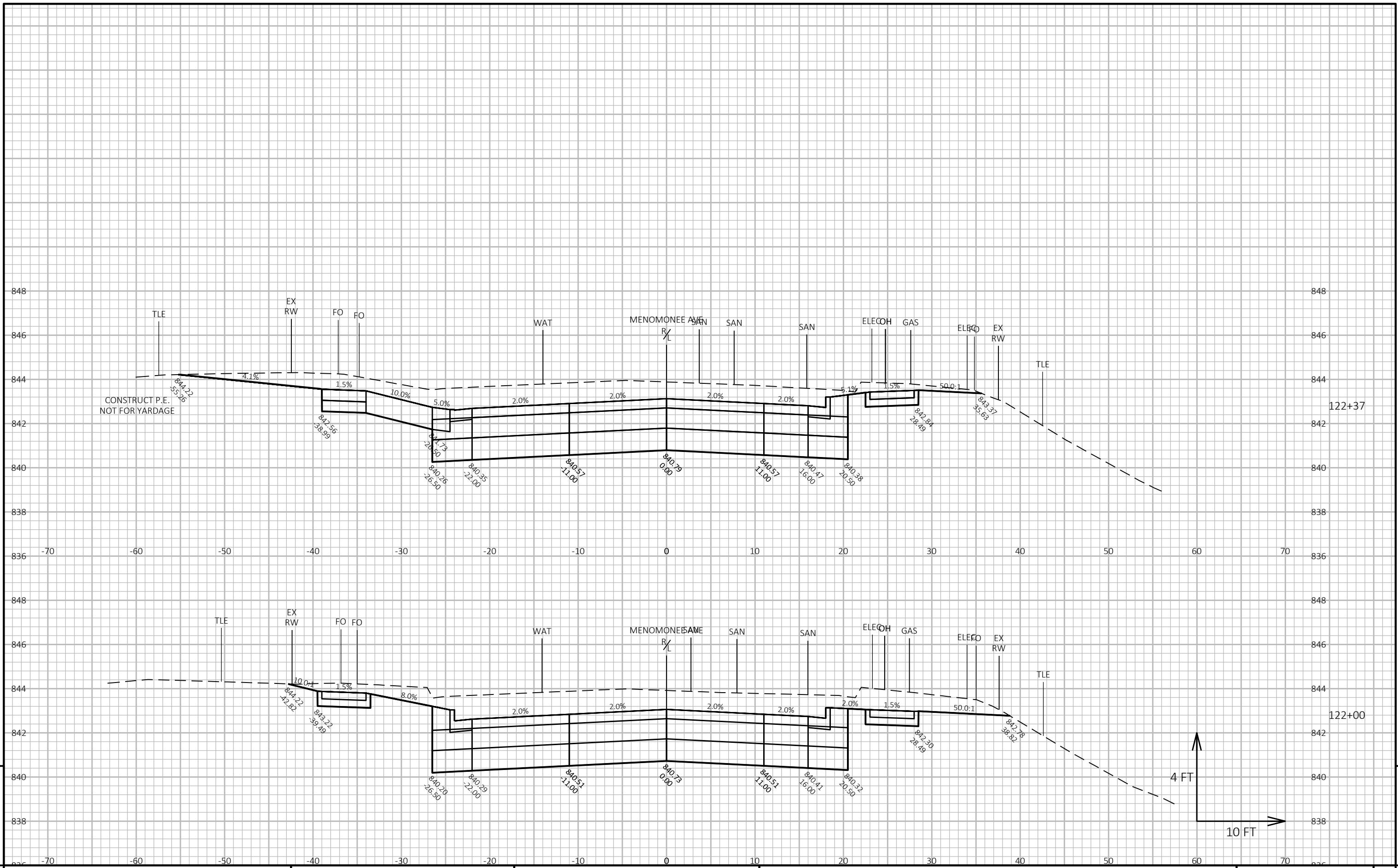


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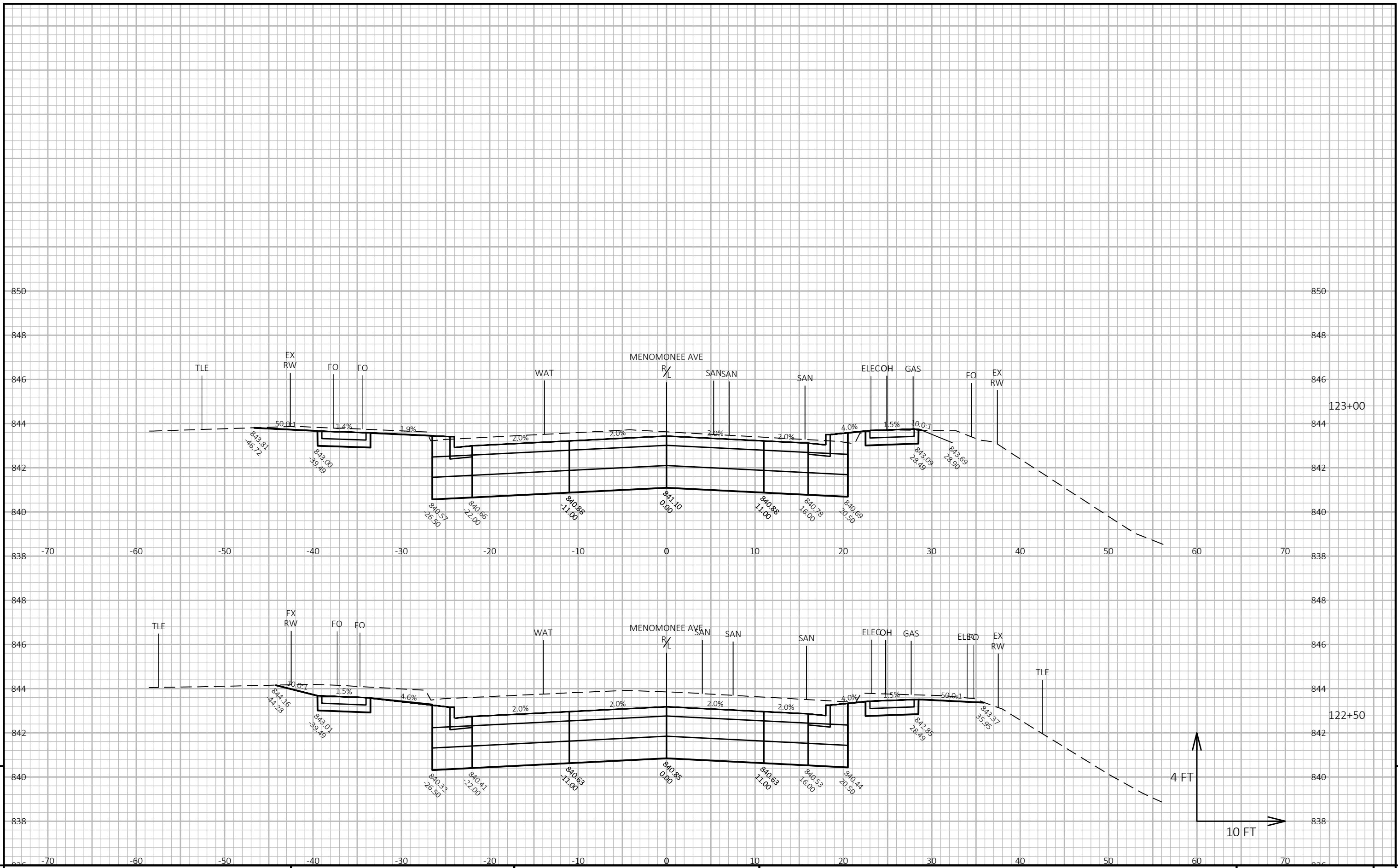
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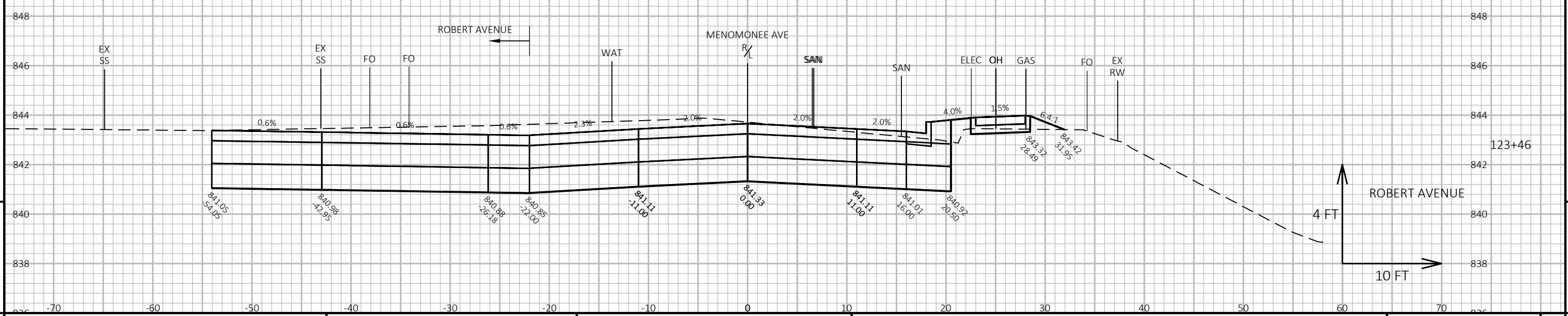
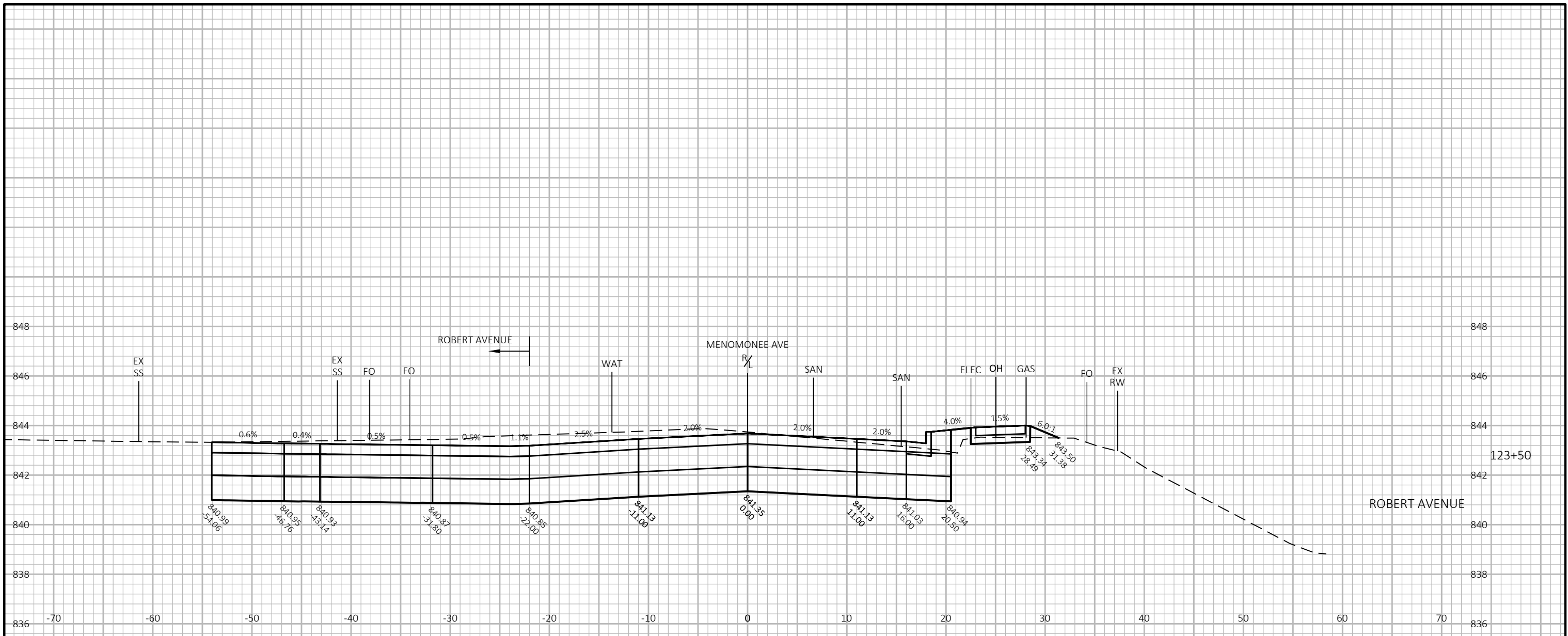
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 PLOT NAME:
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 WISDOT/CADD SHEET 49



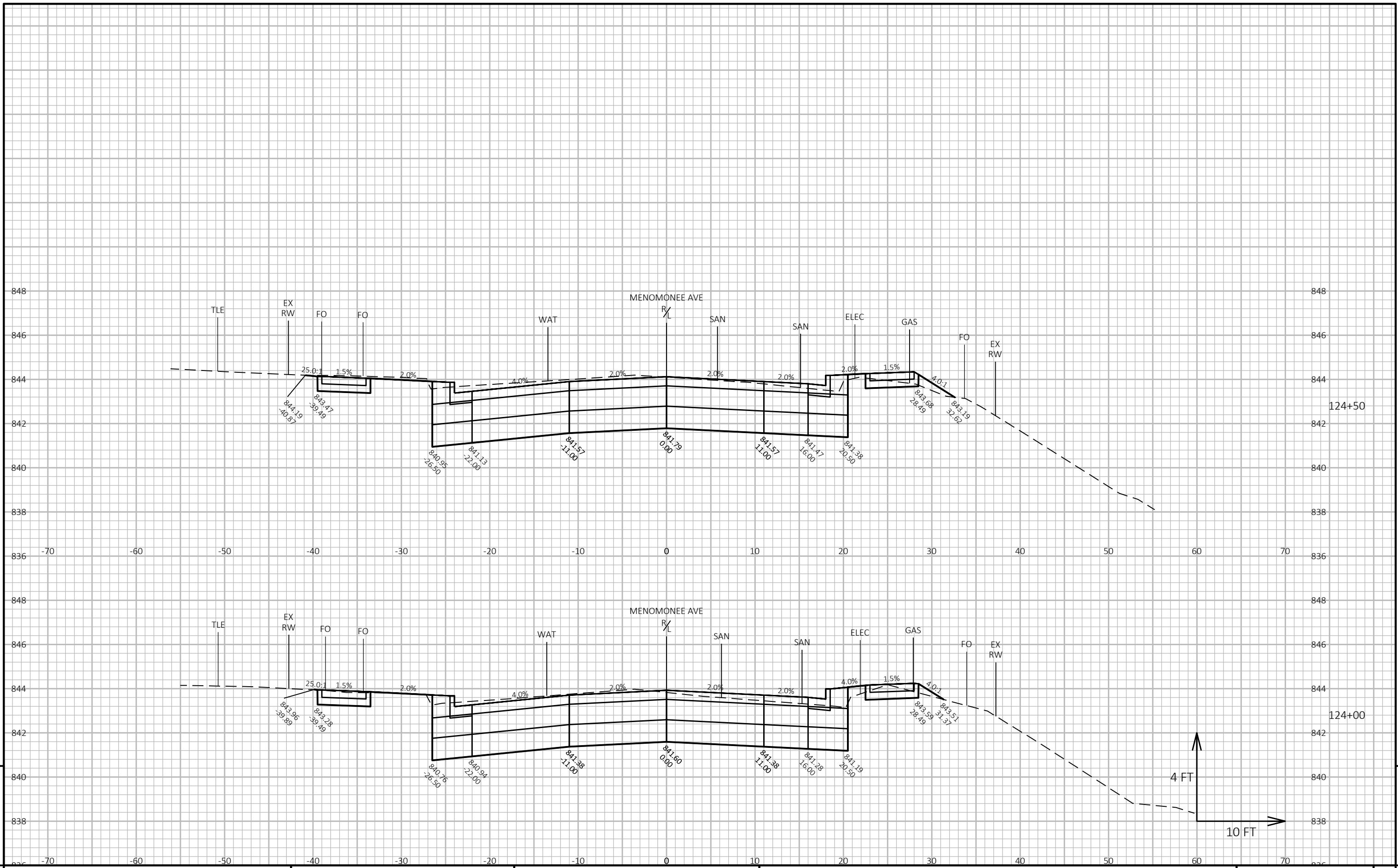
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PROJECT NO: 2720-07-71 HWY: MENOMONEE AVE COUNTY: WAUKESHA CROSS SECTIONS: MENOMONEE AVE SHEET 9



PROJECT NO: 2720-07-71	HWY: MENOMONEE AVE	COUNTY: WAUKESHA	CROSS SECTIONS: MENOMONEE AVE	SHEET	E
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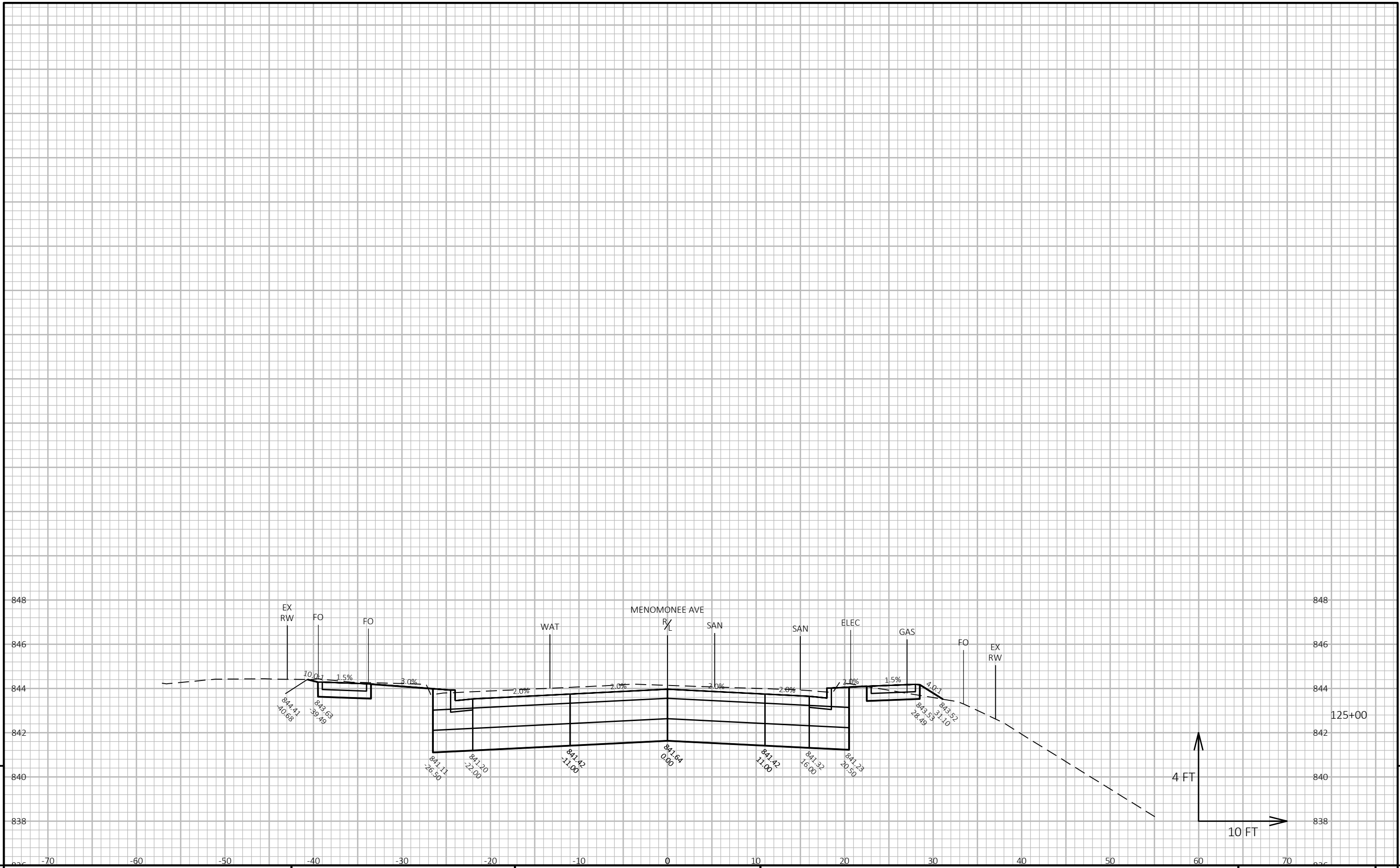
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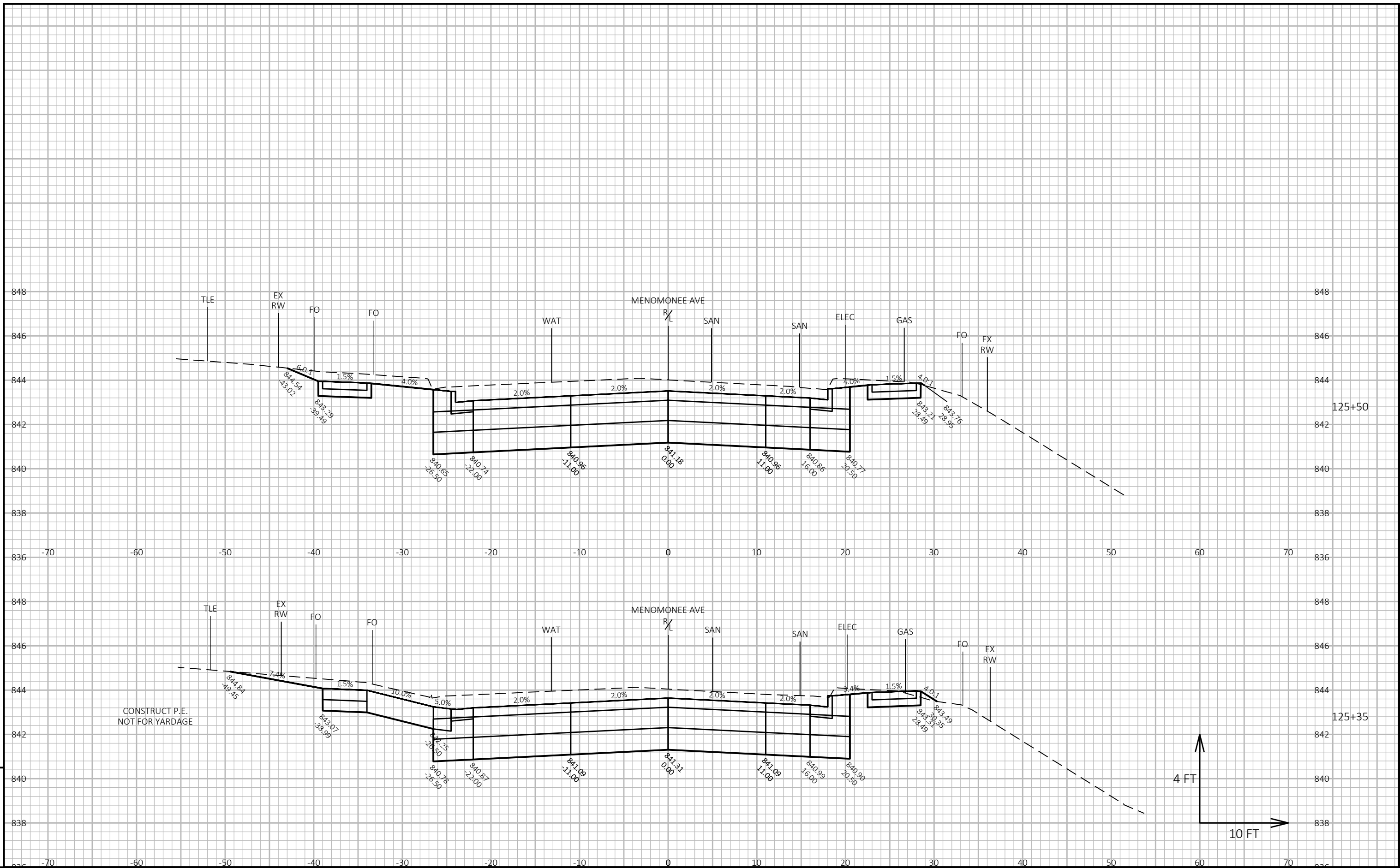
PROJECT NO: 2720-07-71 HWY: MENOMONEE AVE COUNTY: WAUKESHA CROSS SECTIONS: MENOMONEE AVE SHEET E

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LAYOUT NAME - 27



PROJECT NO: 2720-07-71 HWY: MENOMONEE AVE COUNTY: WAUKESHA CROSS SECTIONS: MENOMONEE AVE SHEET E



PROJECT NO: 2720-07-71

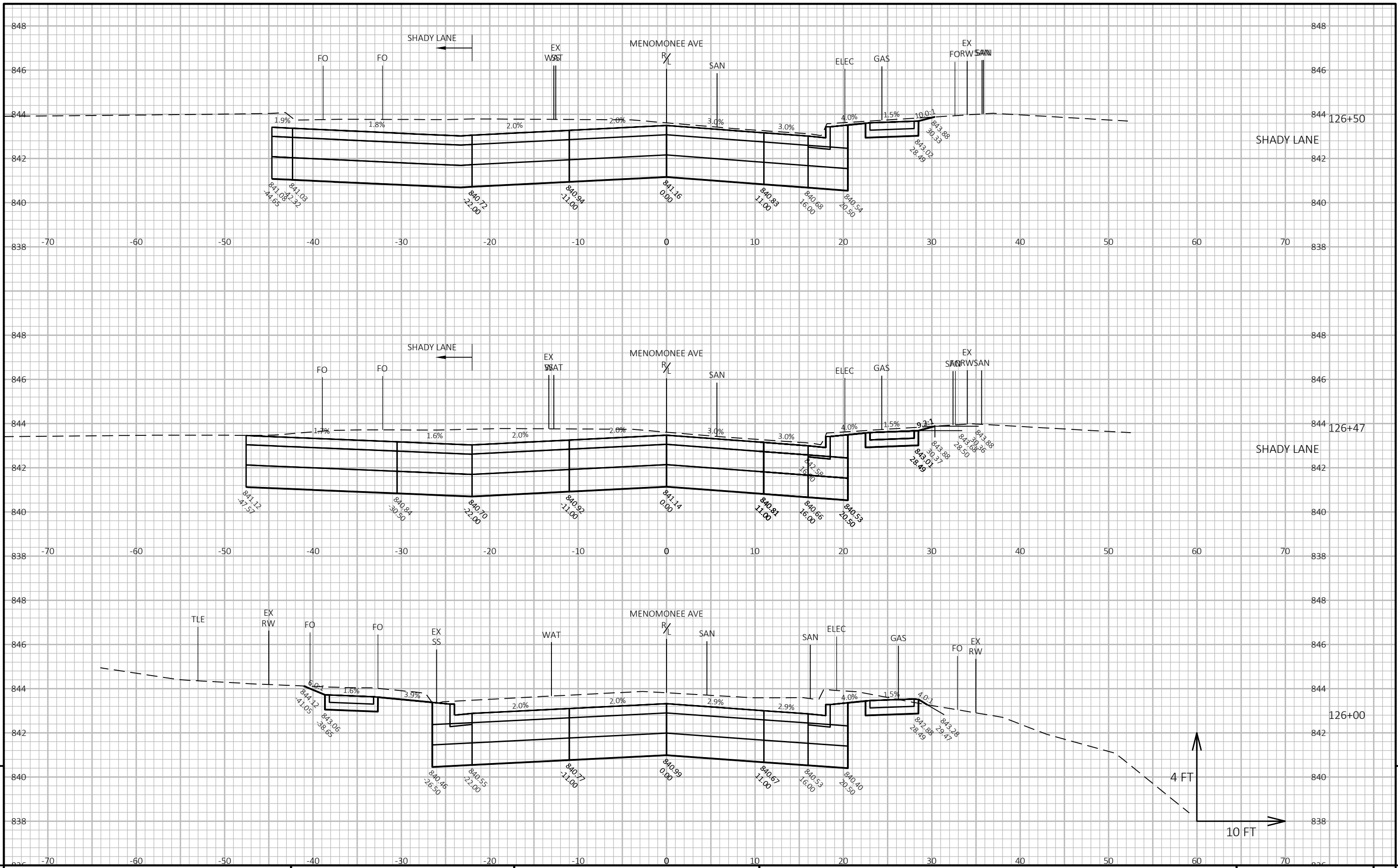
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COUNTY: WAUKESHA

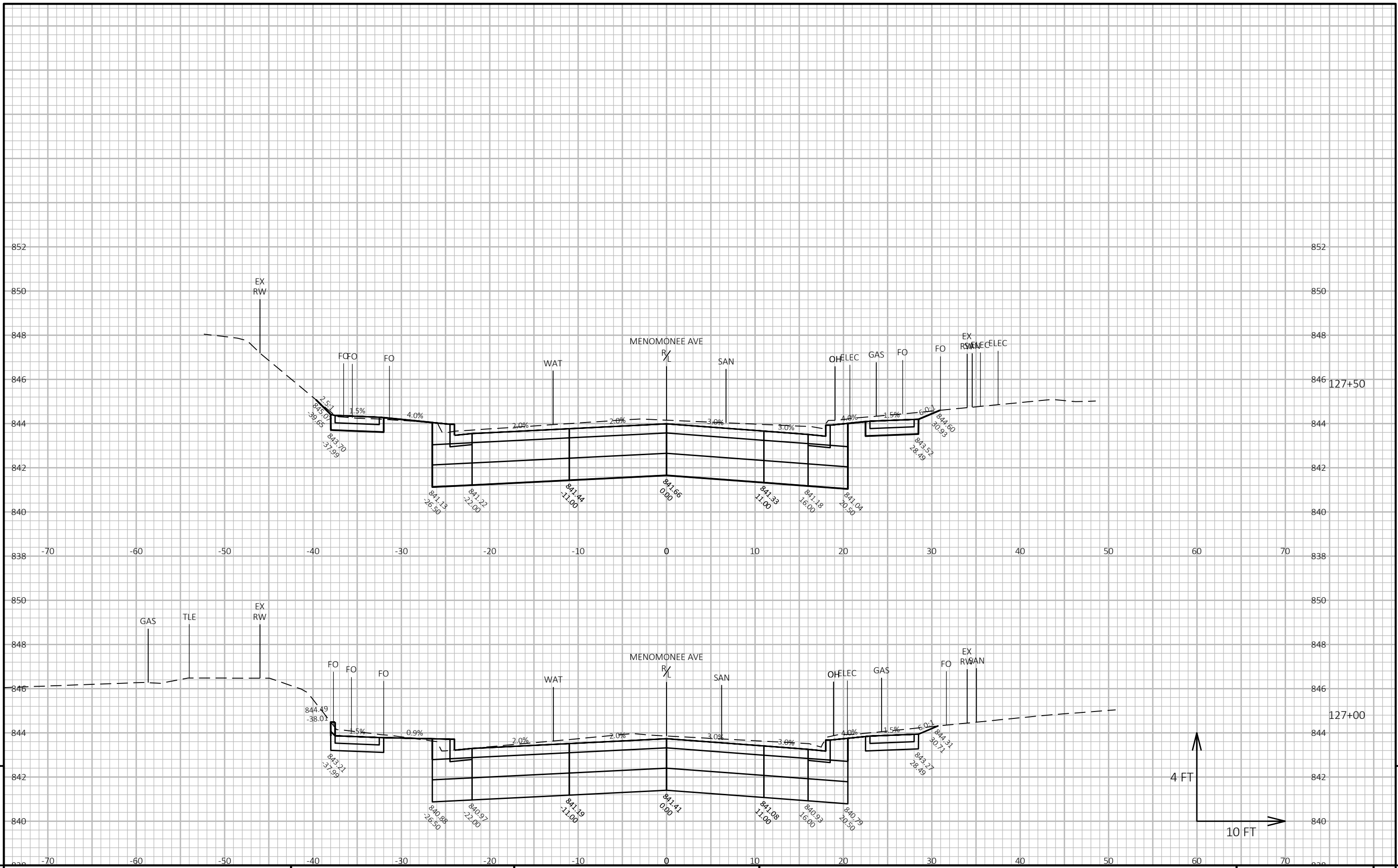
CROSS SECTIONS: MENOMONEE AVE

SHEET

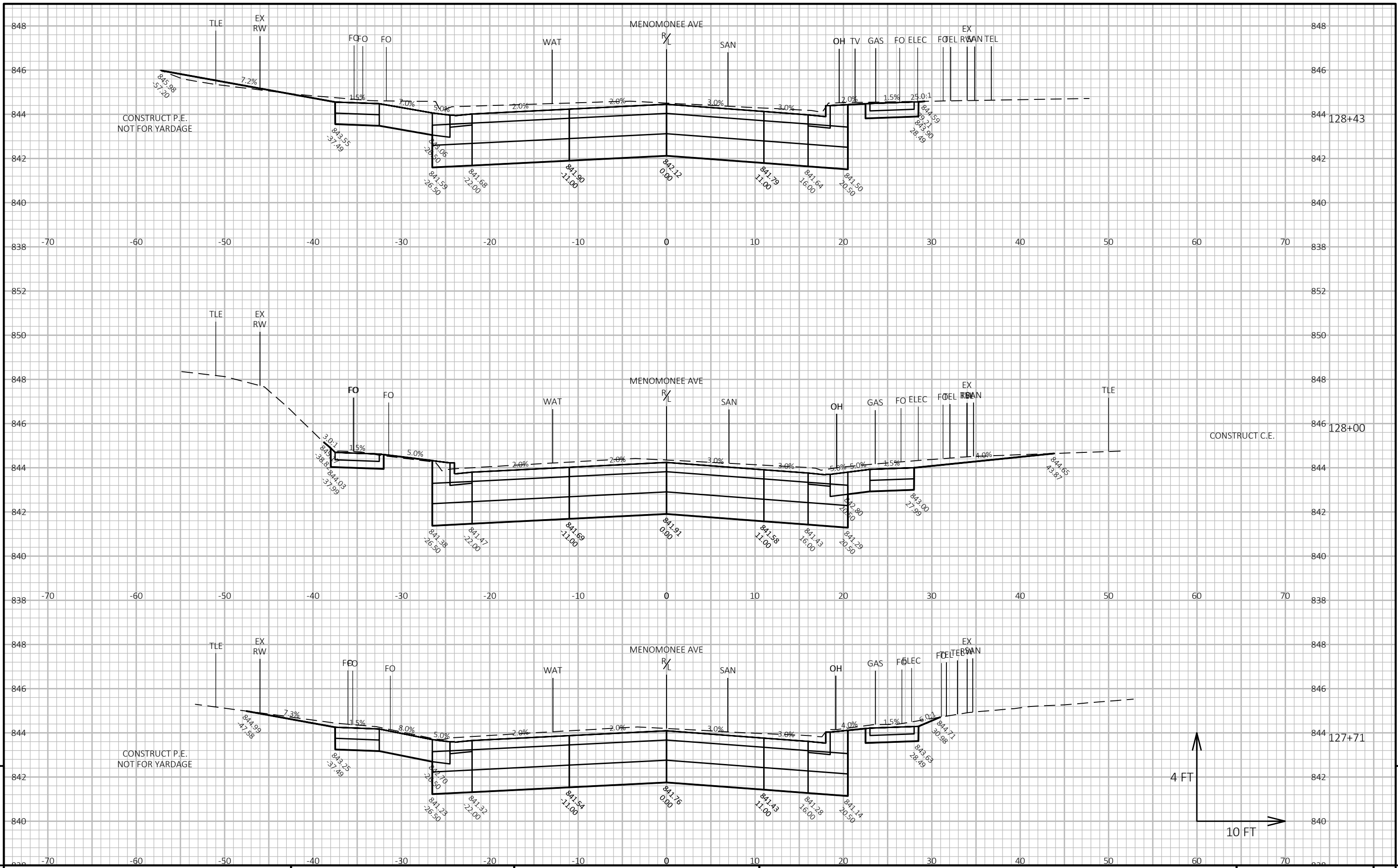
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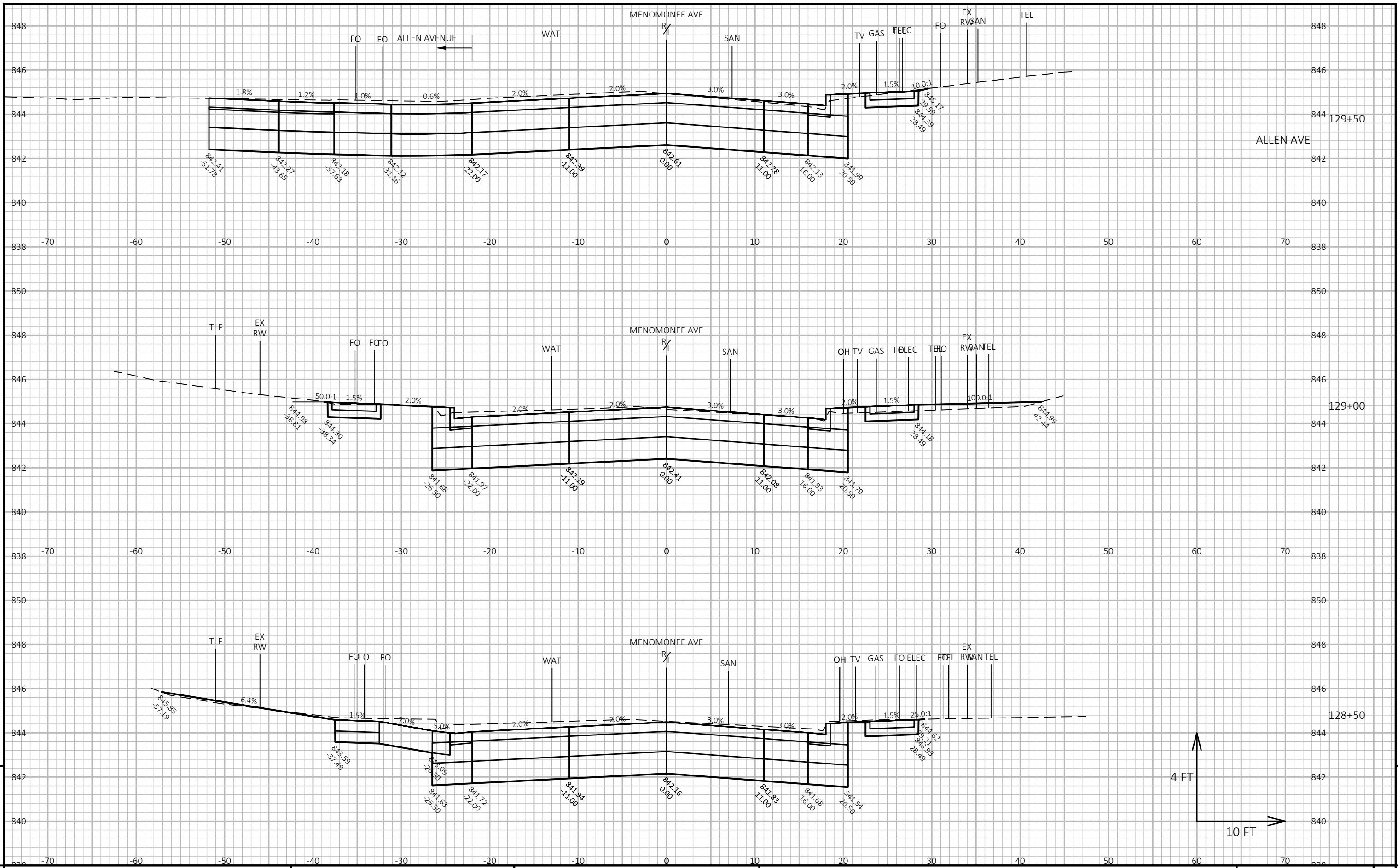
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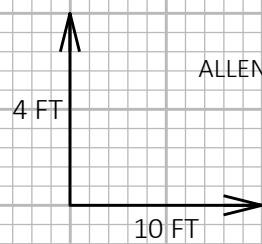
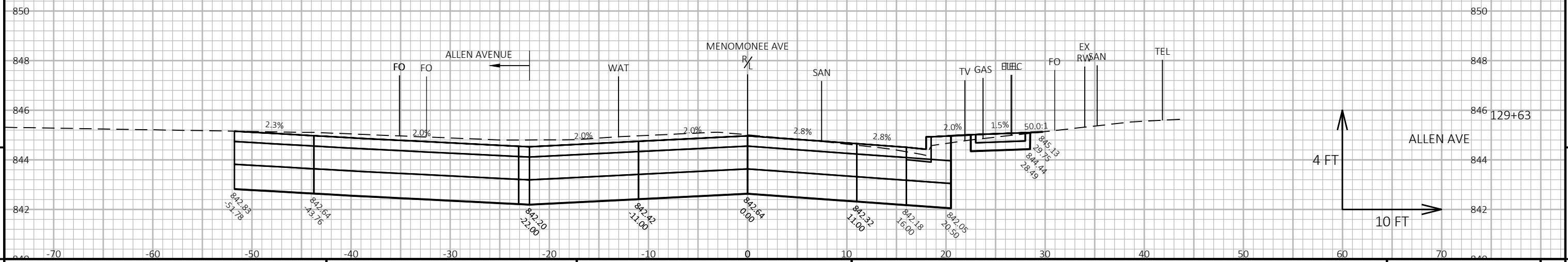
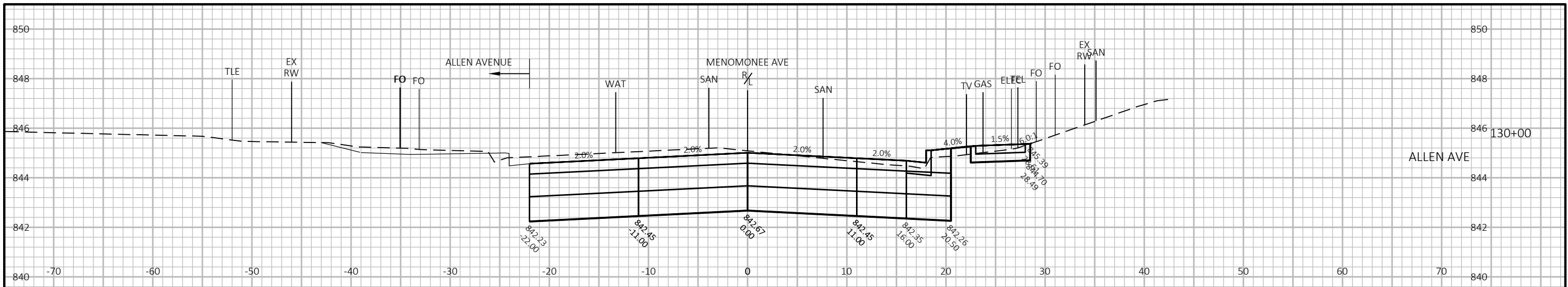
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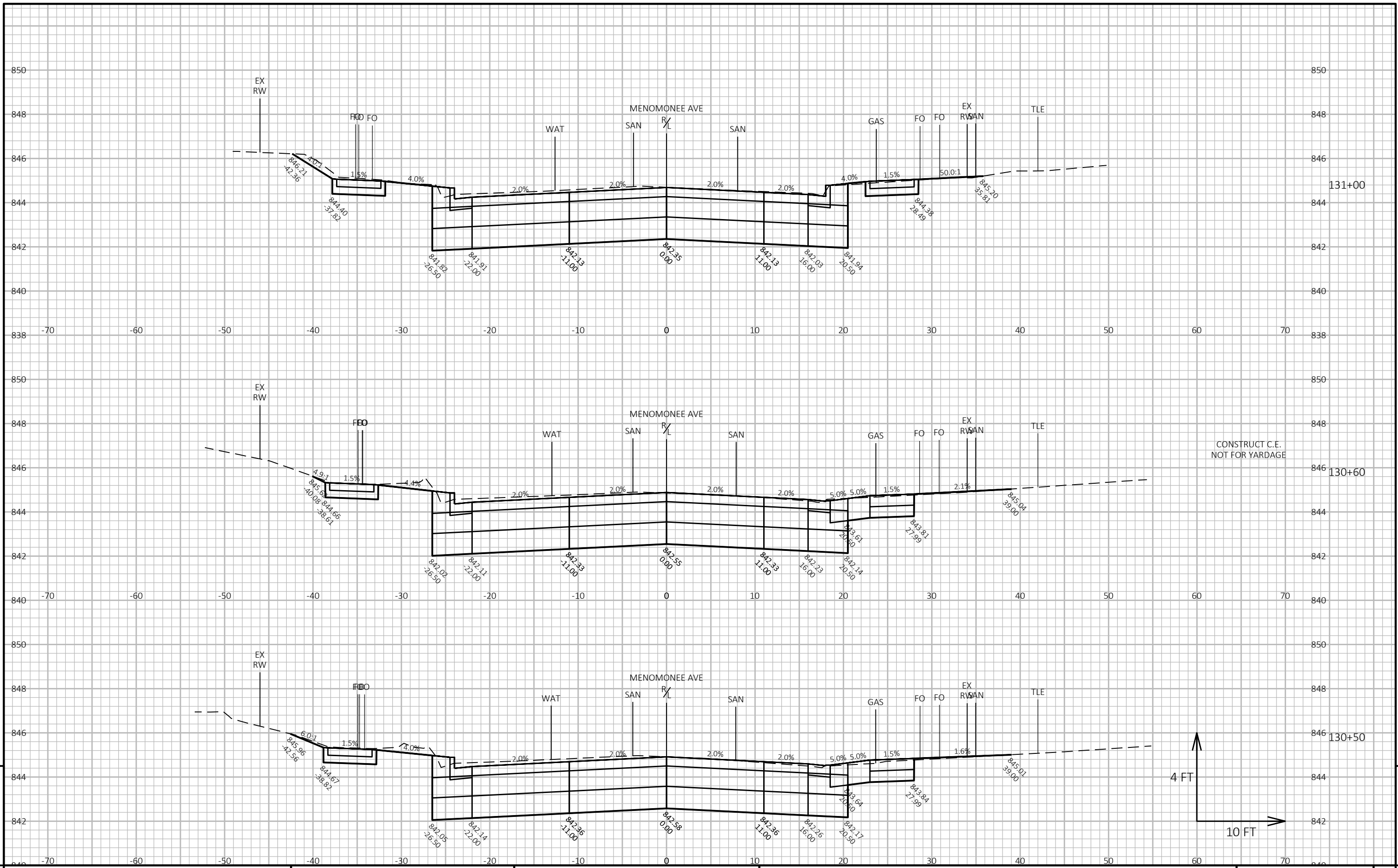
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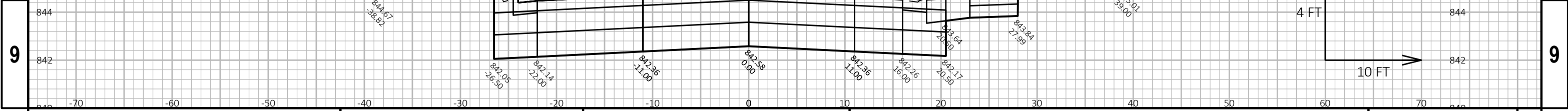
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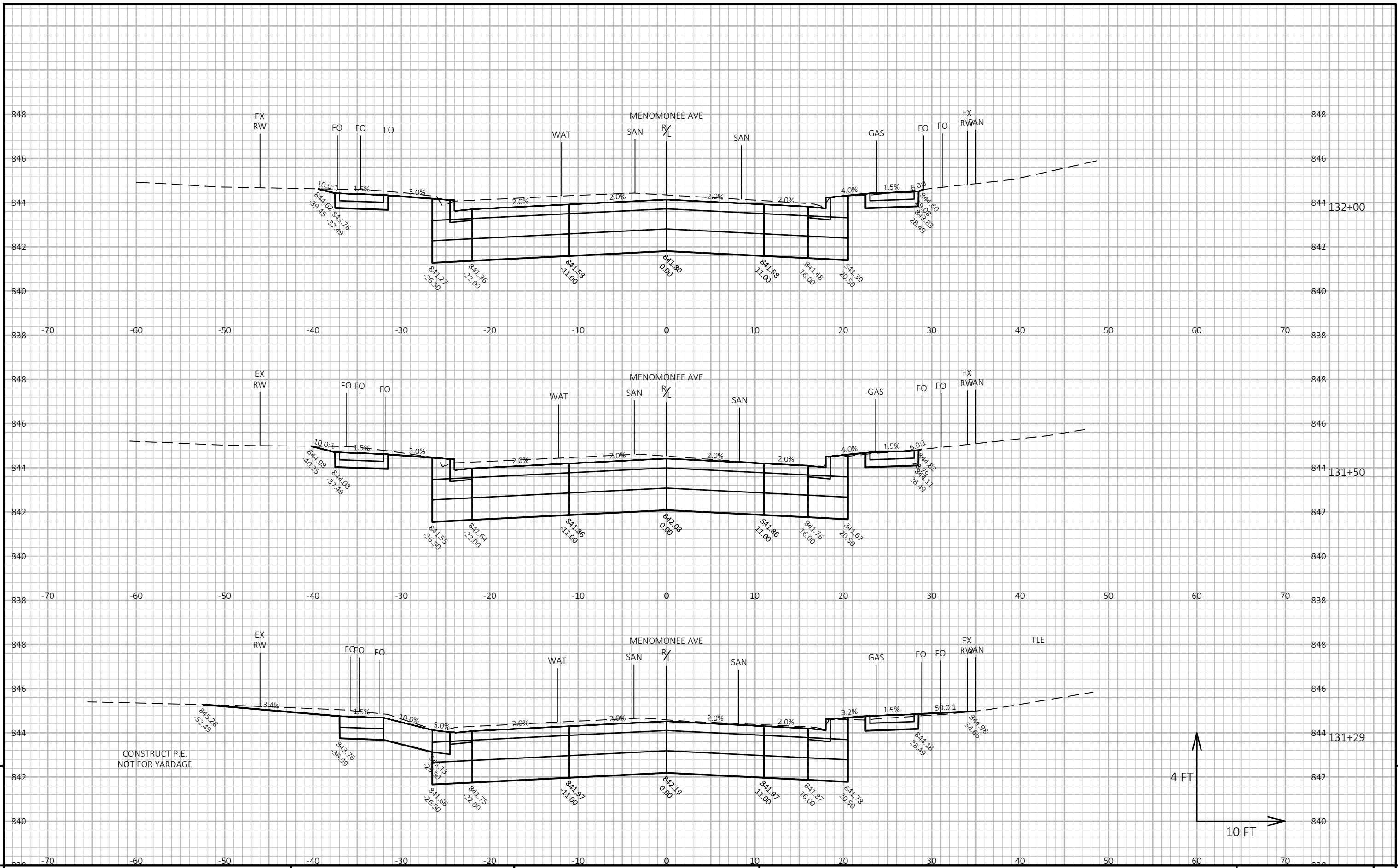
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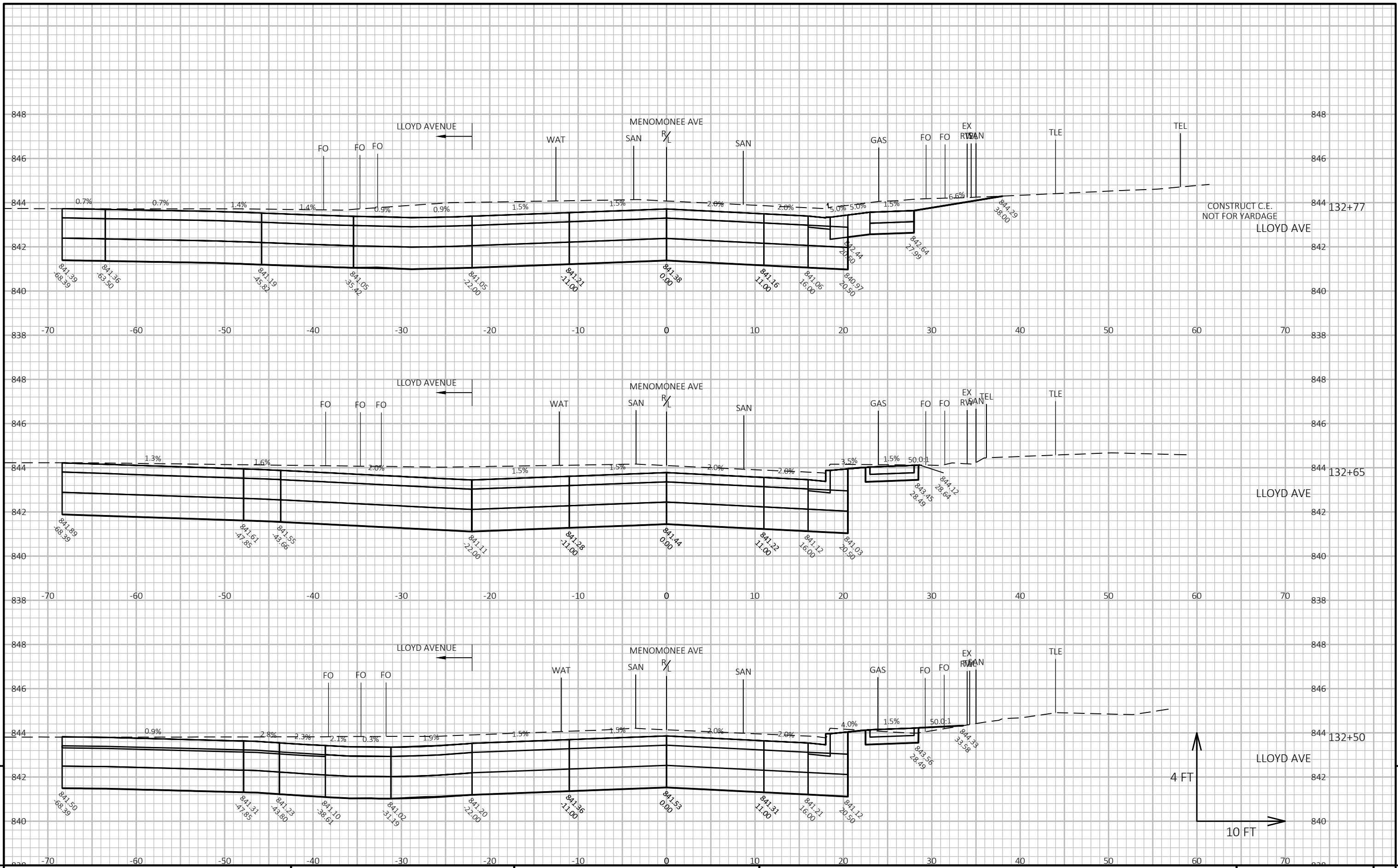
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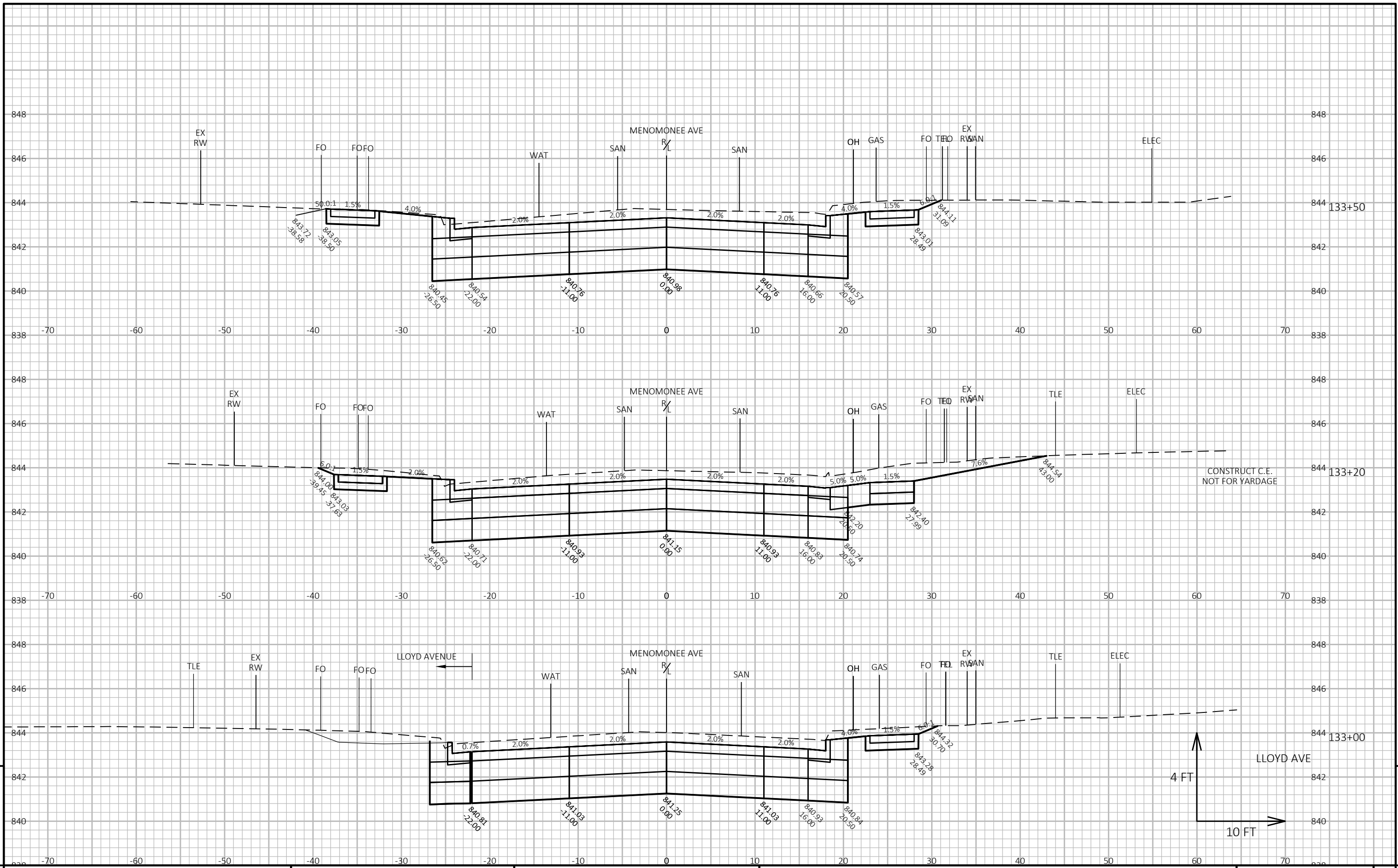
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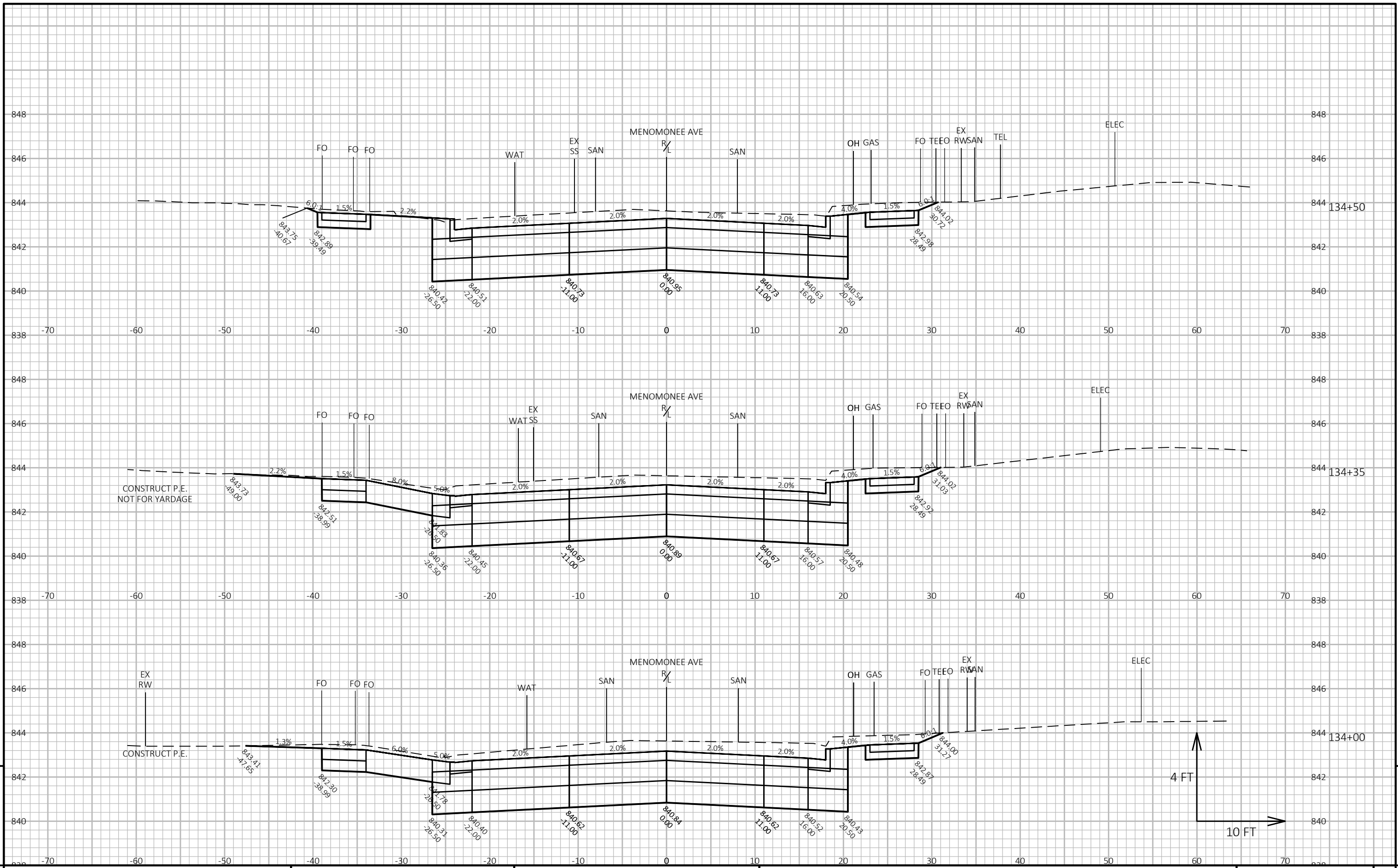
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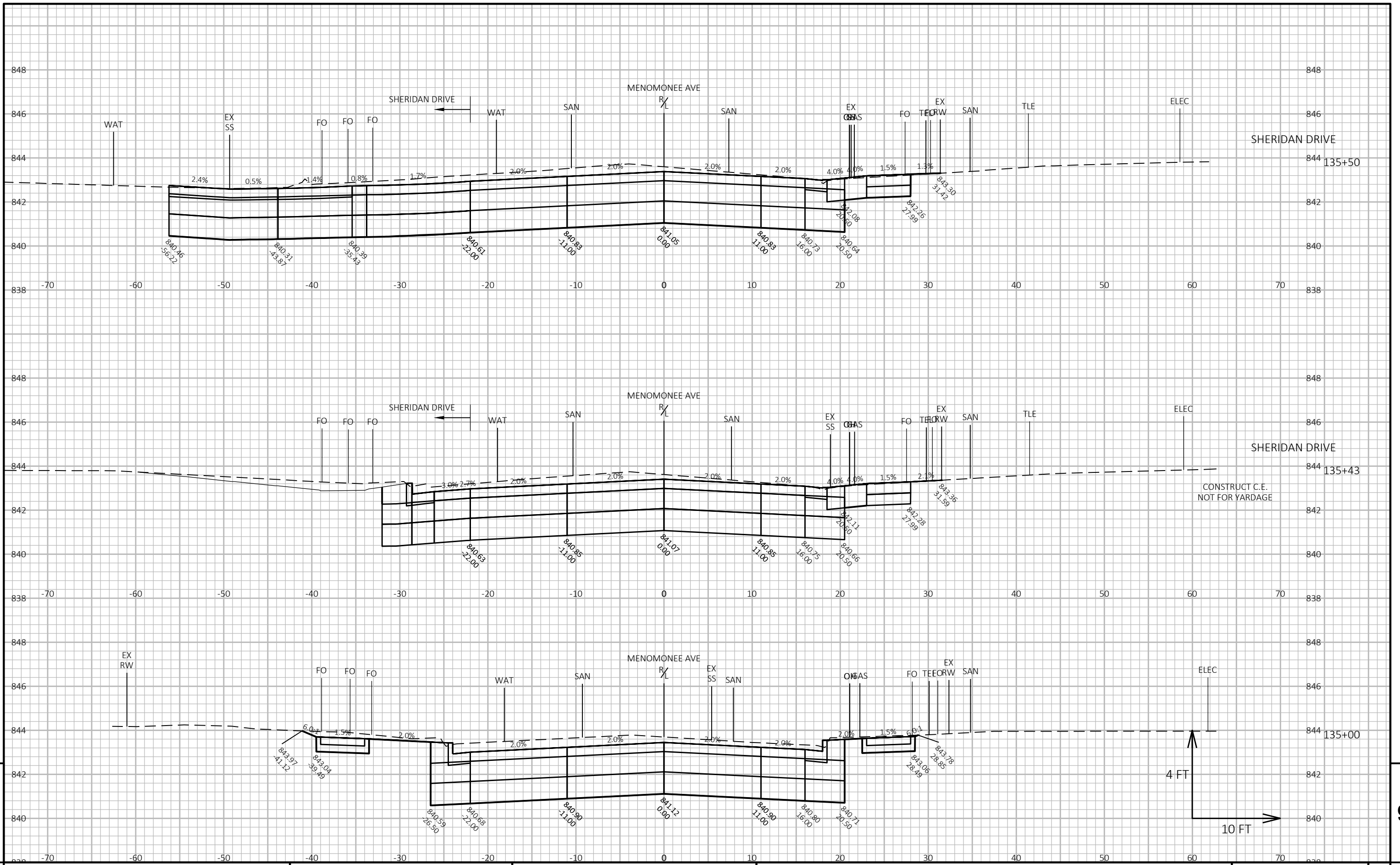
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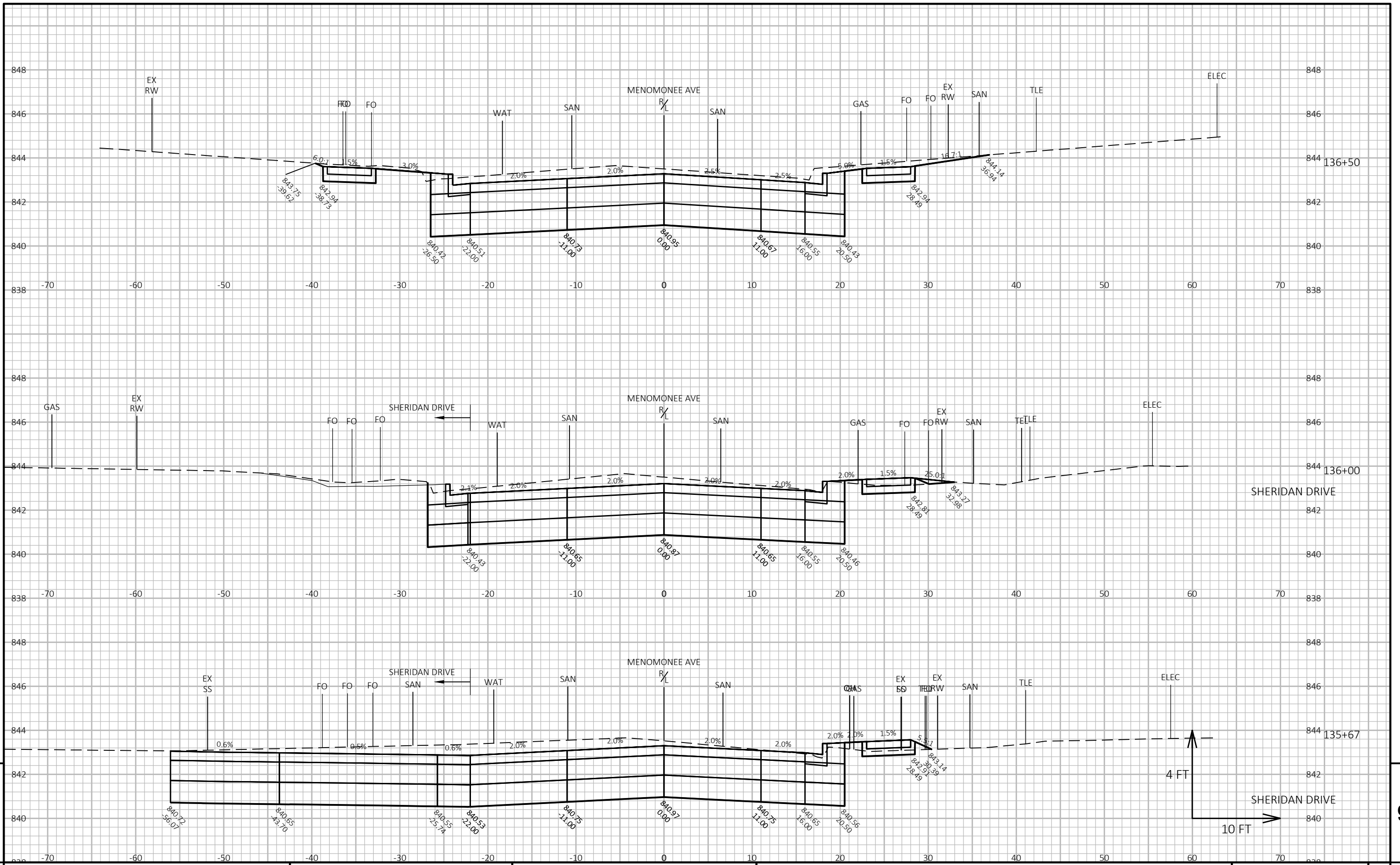
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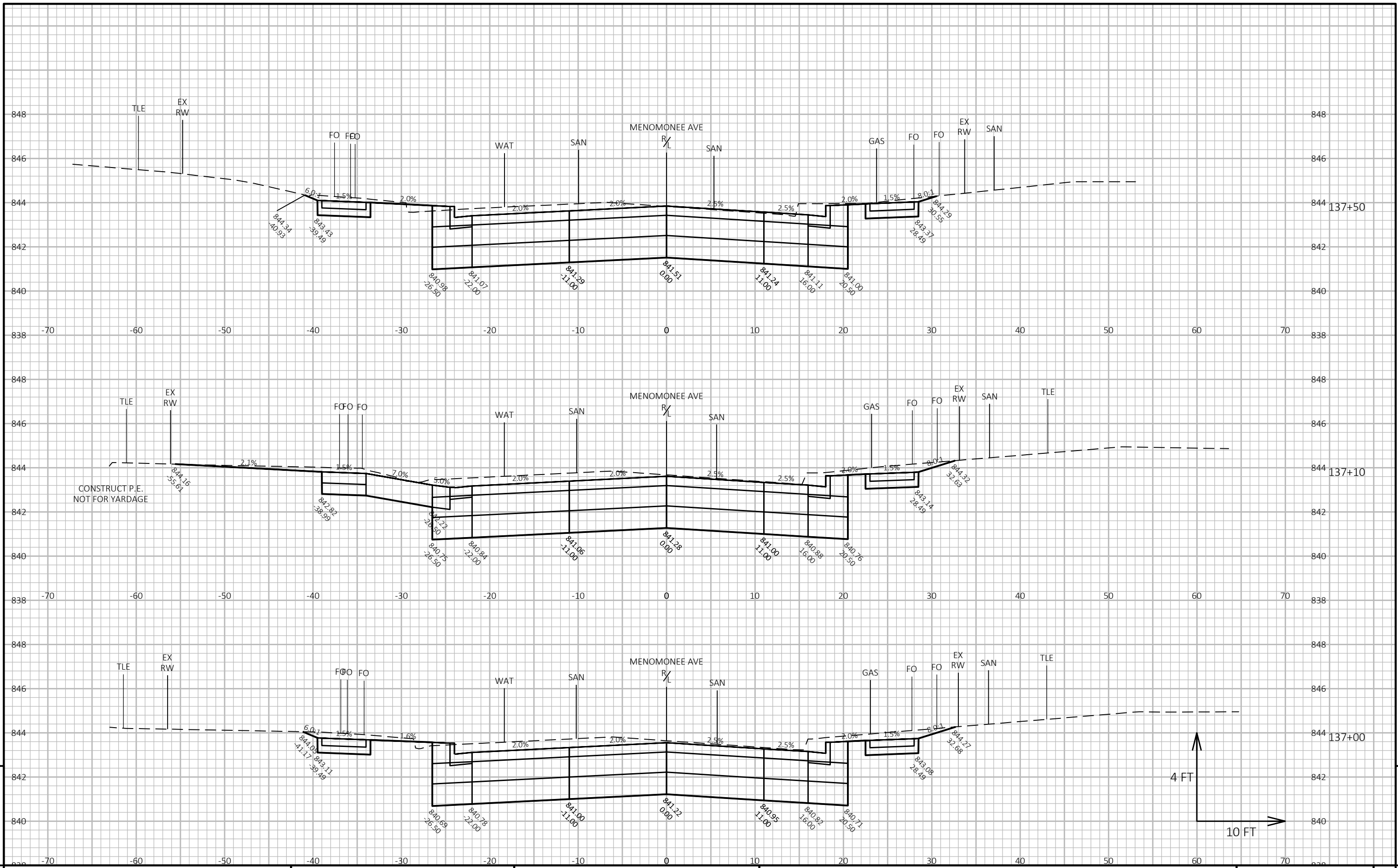
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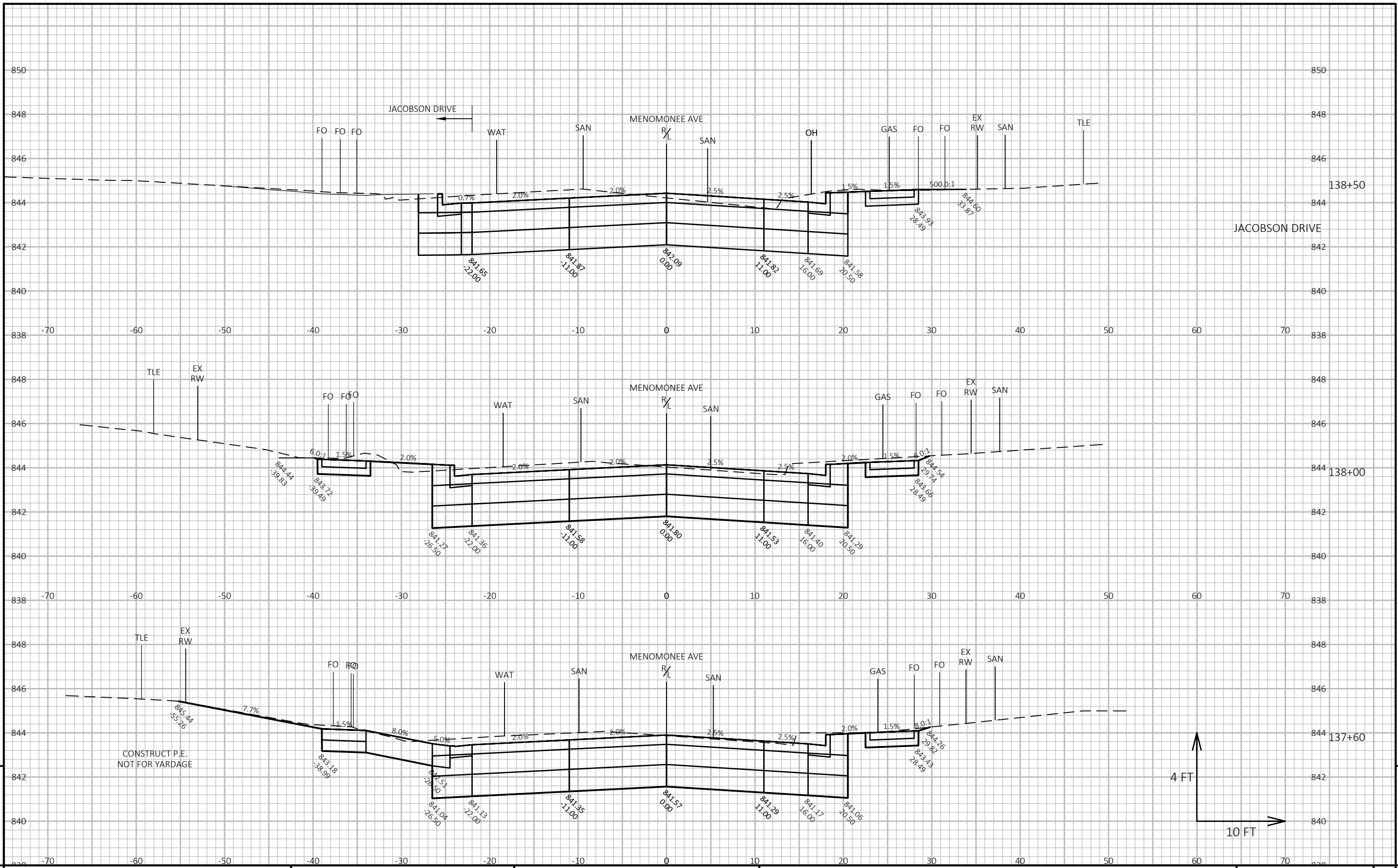
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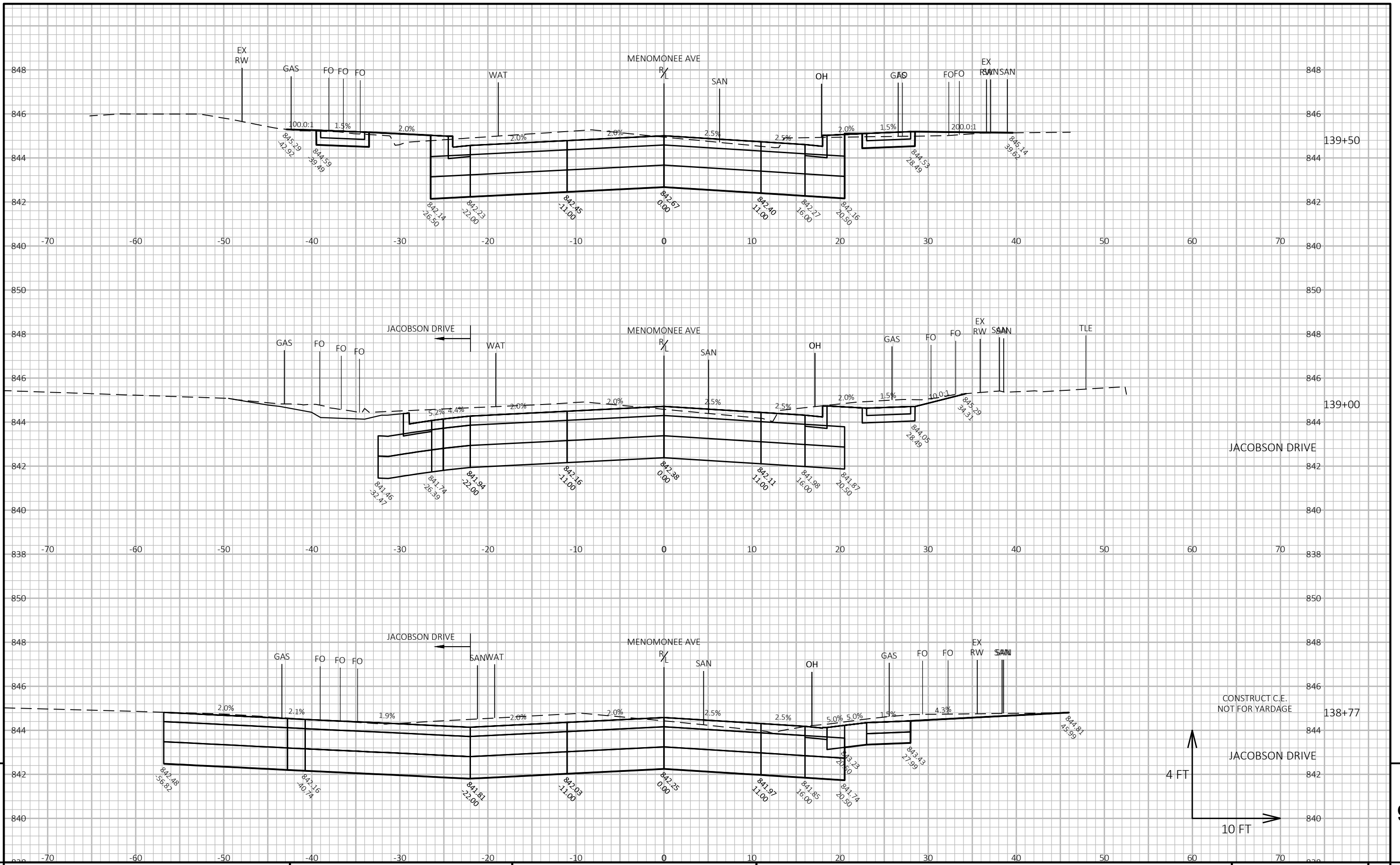
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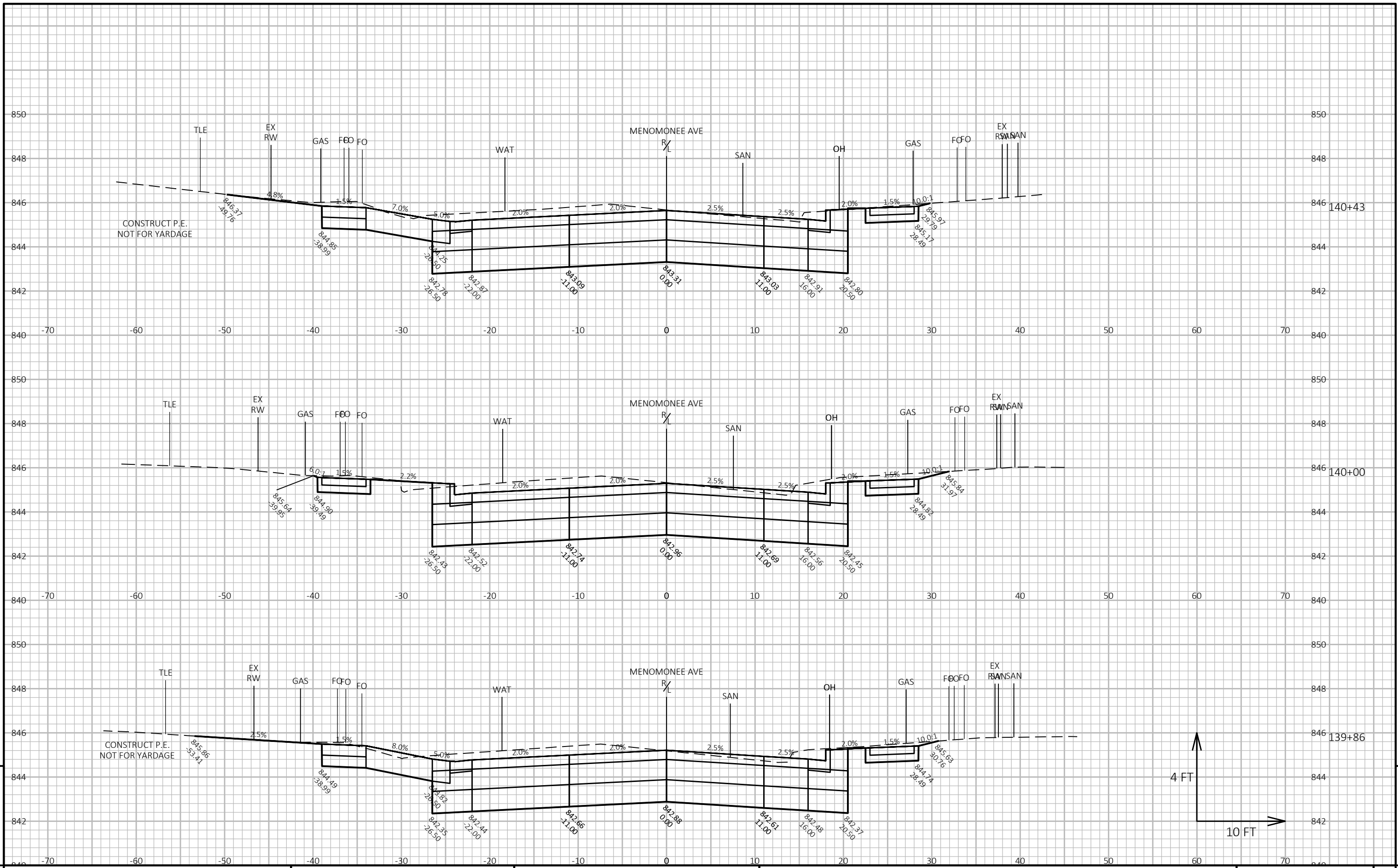
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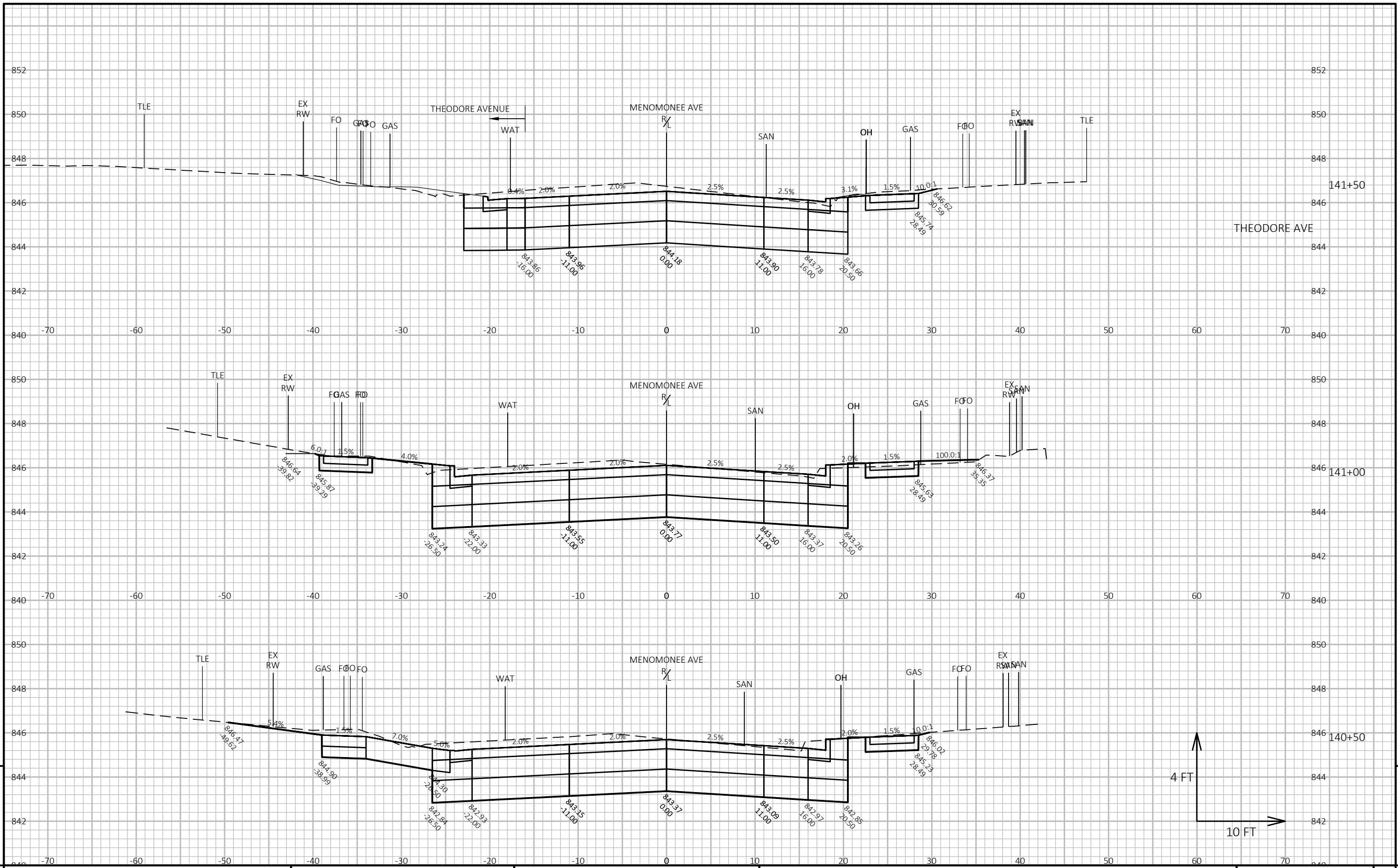
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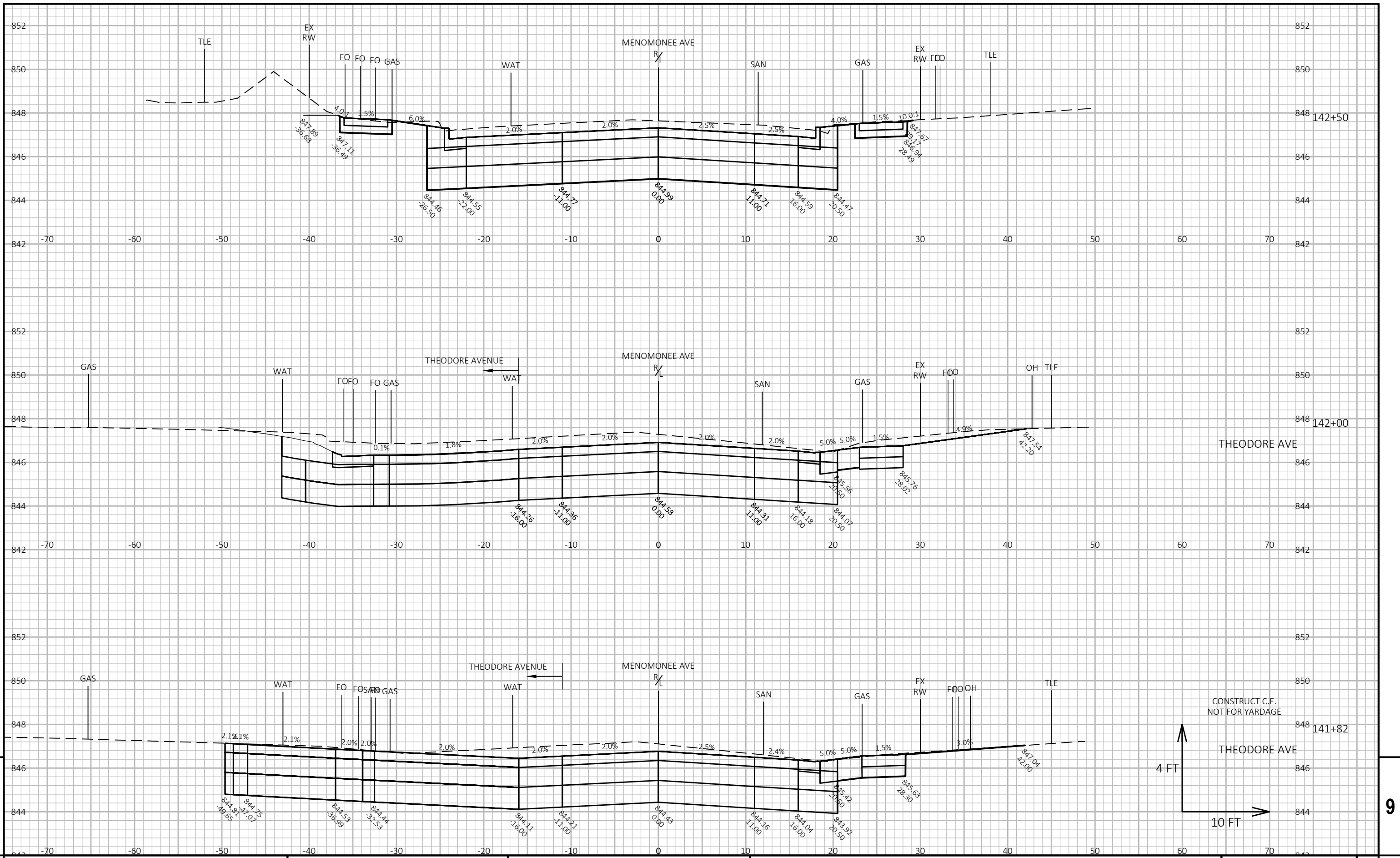
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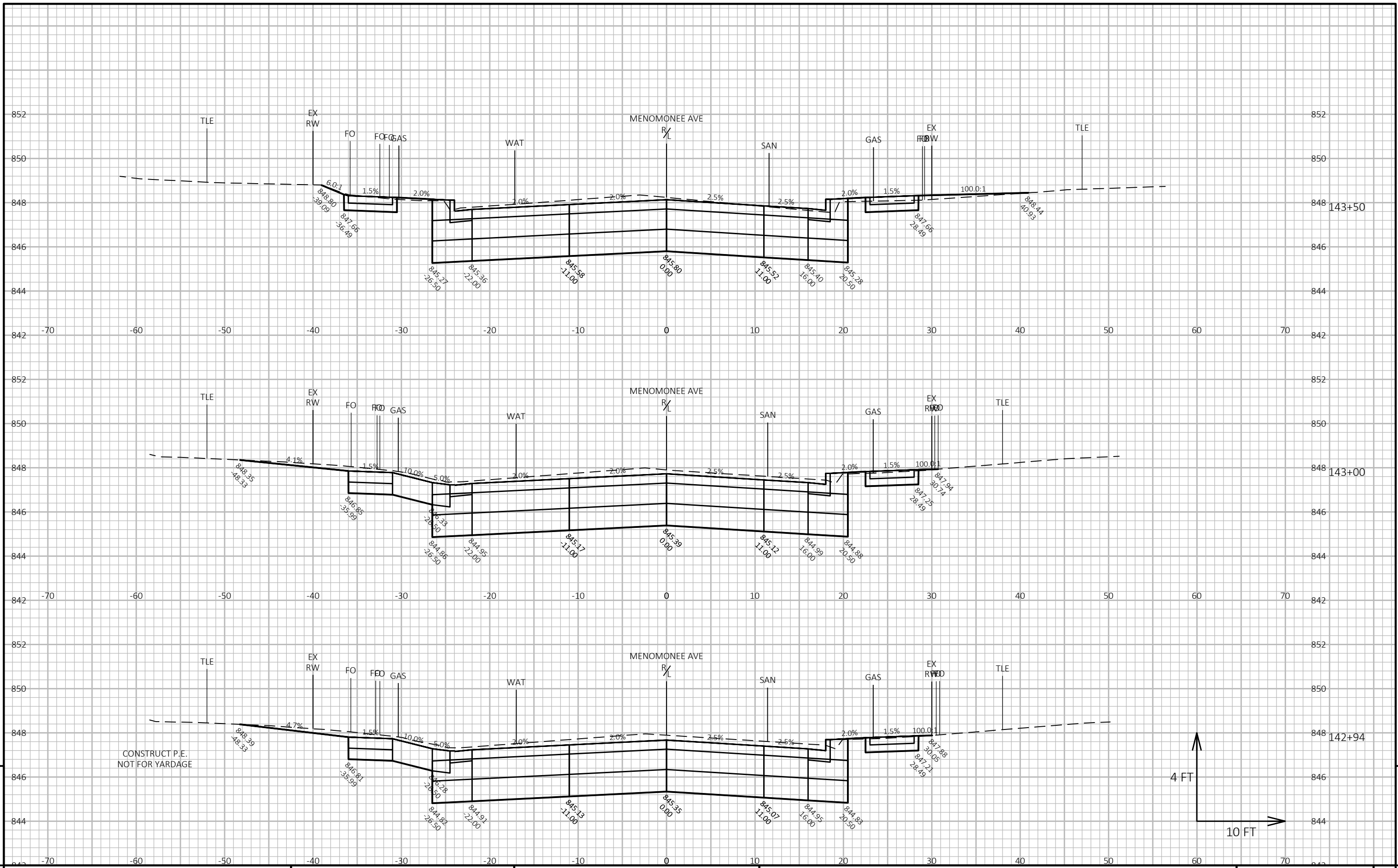
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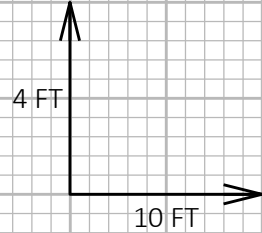
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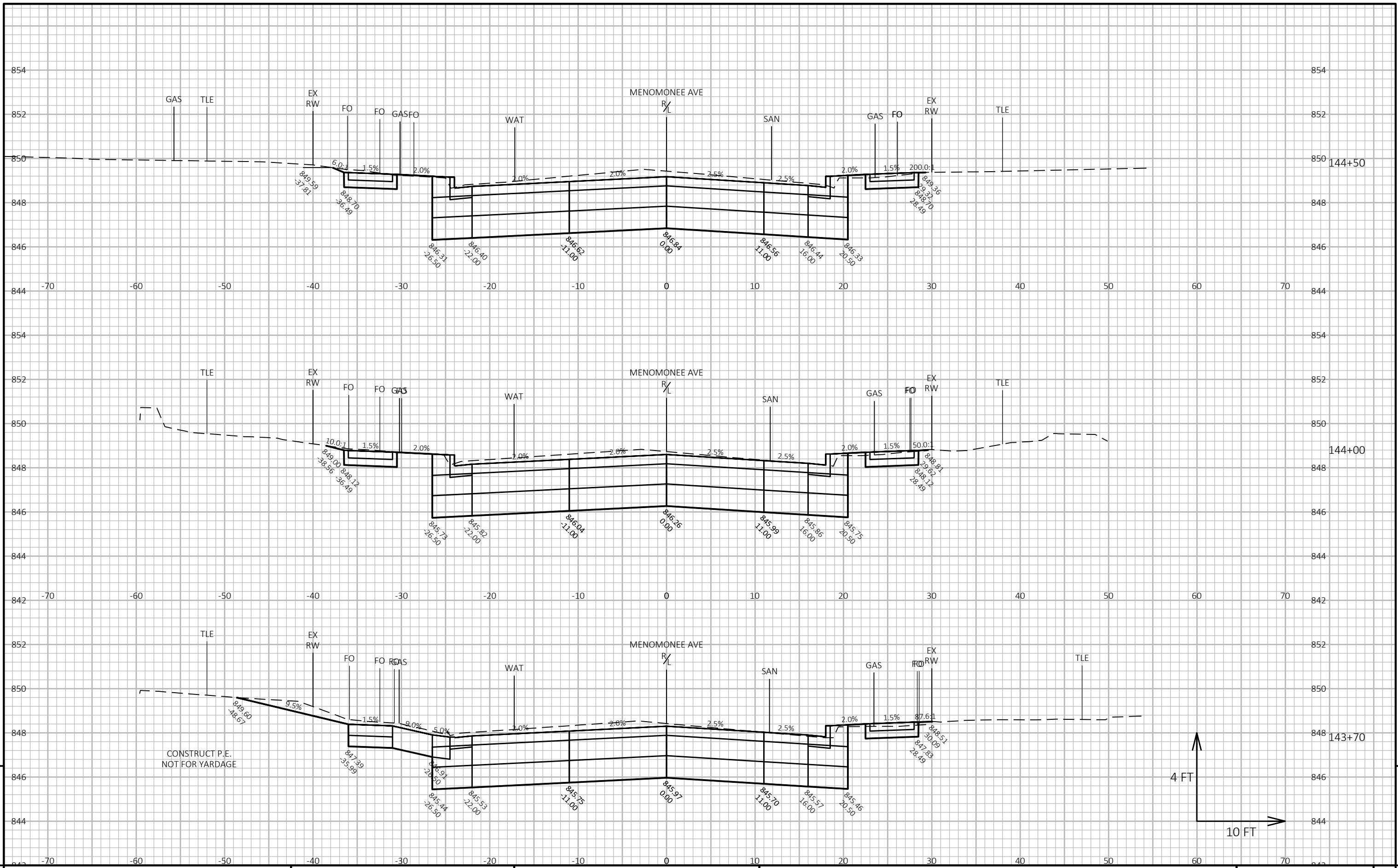


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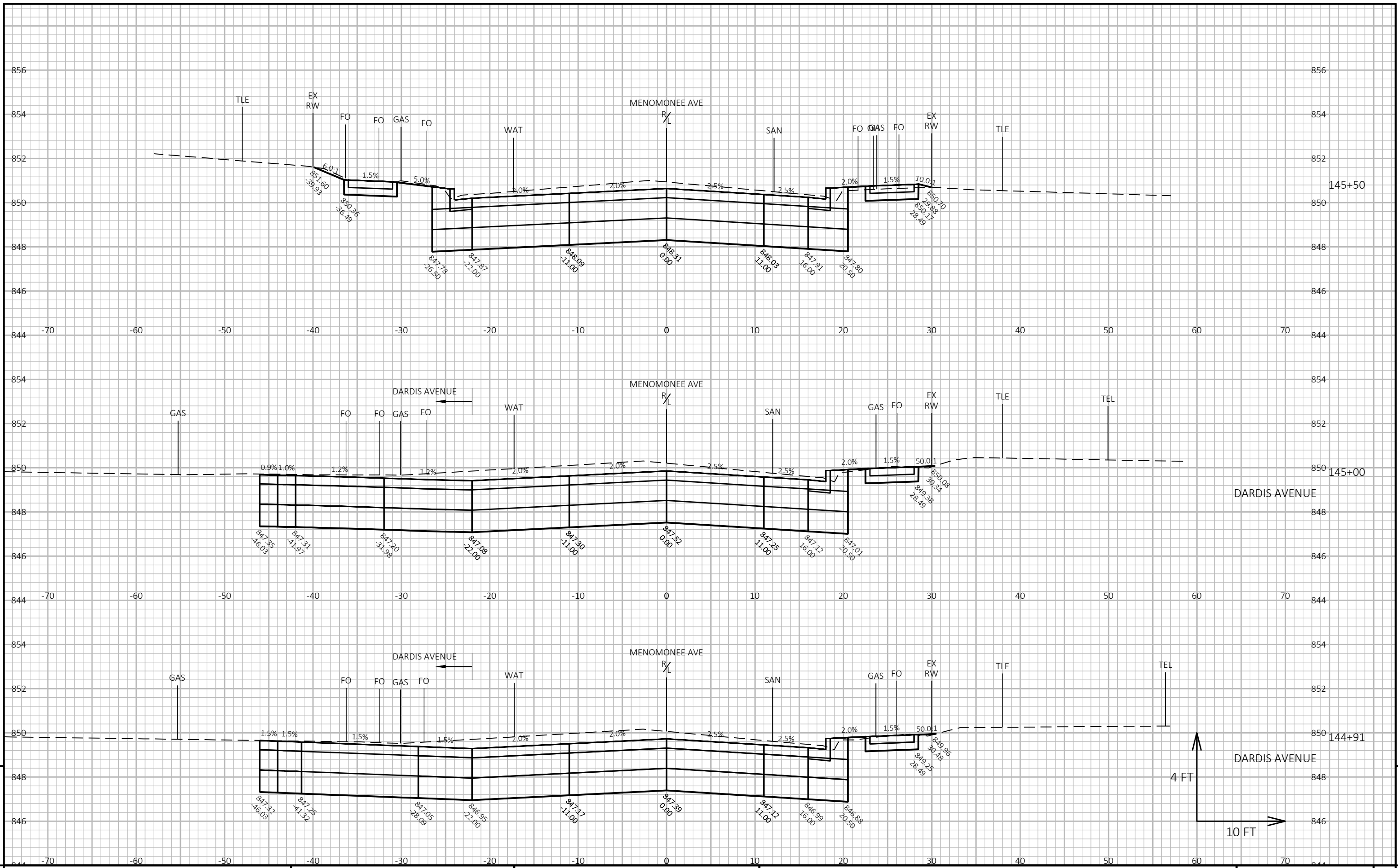


CONSTRUCT P.E.
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PROJECT NO: 2720-07-71

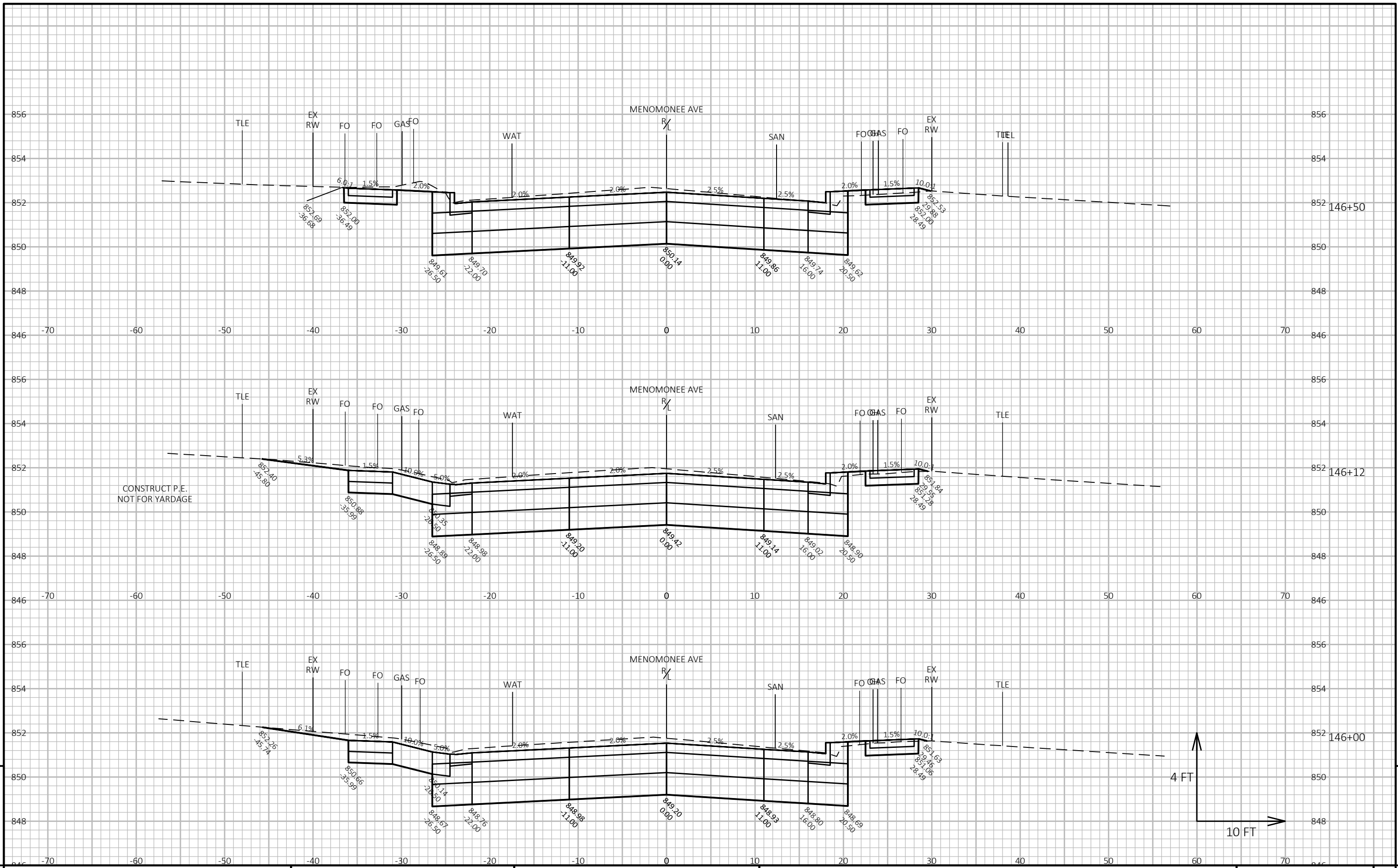
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COUNTY: WAUKESHA

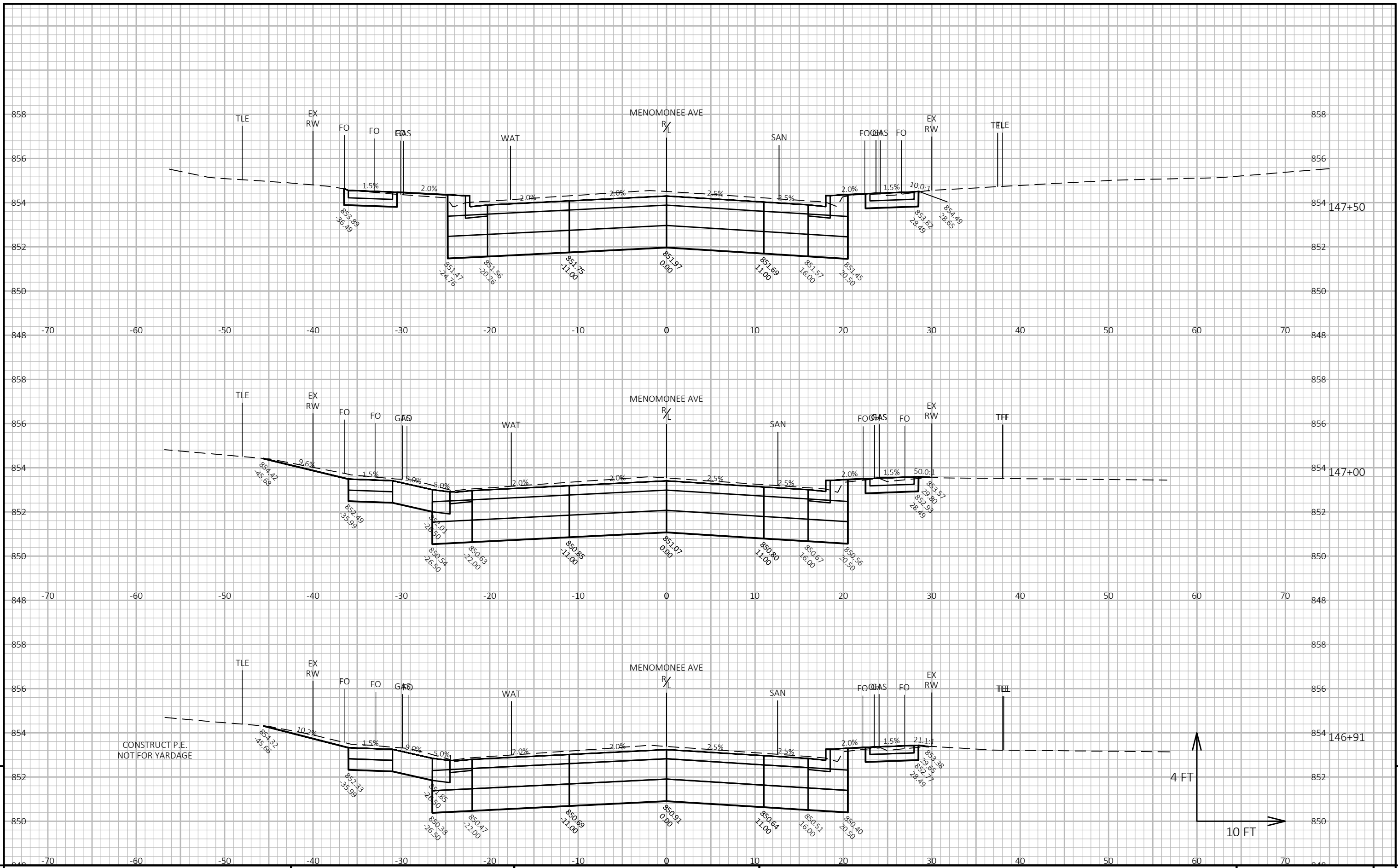
CROSS SECTIONS: MENOMONEE AVE

SHEET

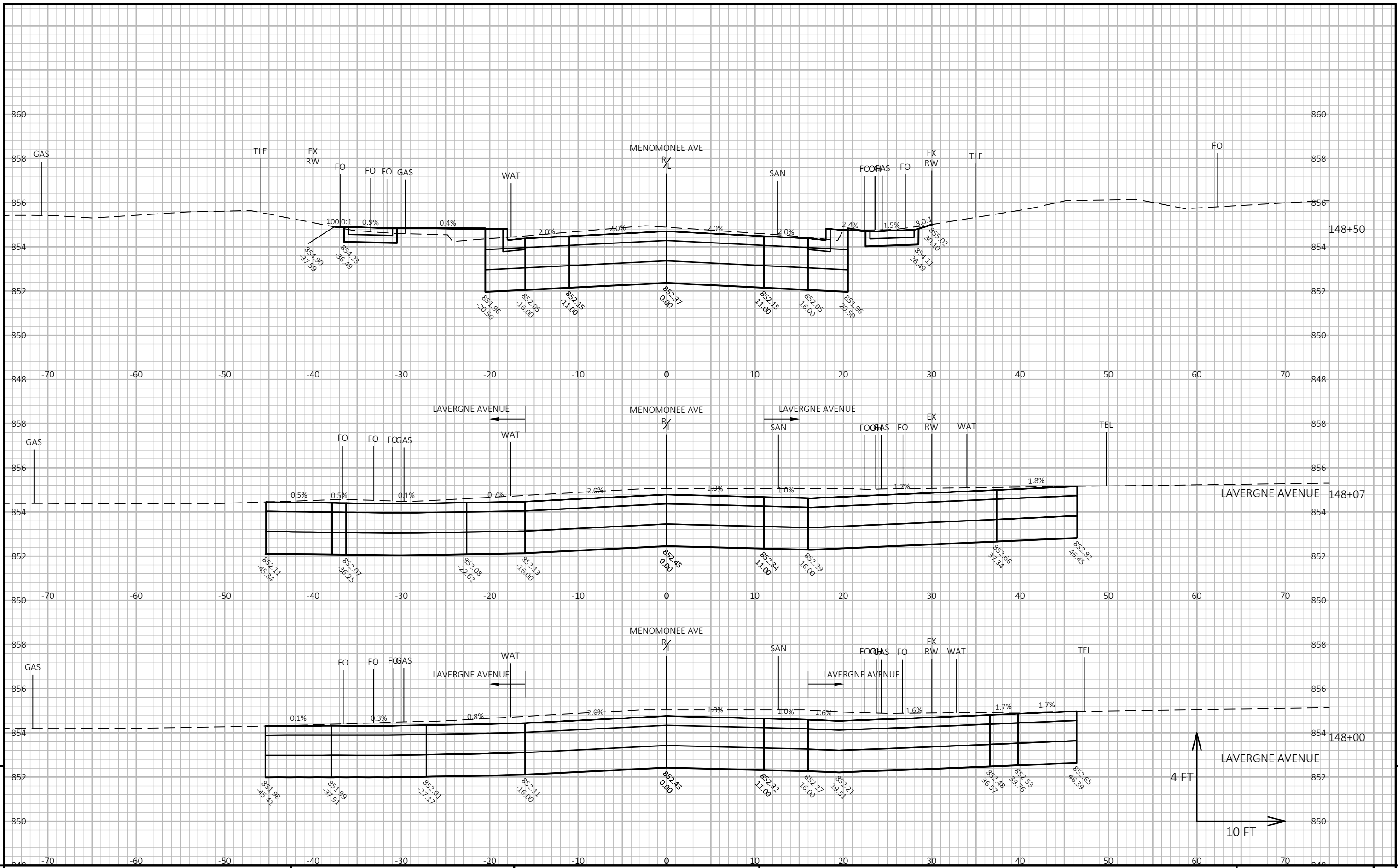
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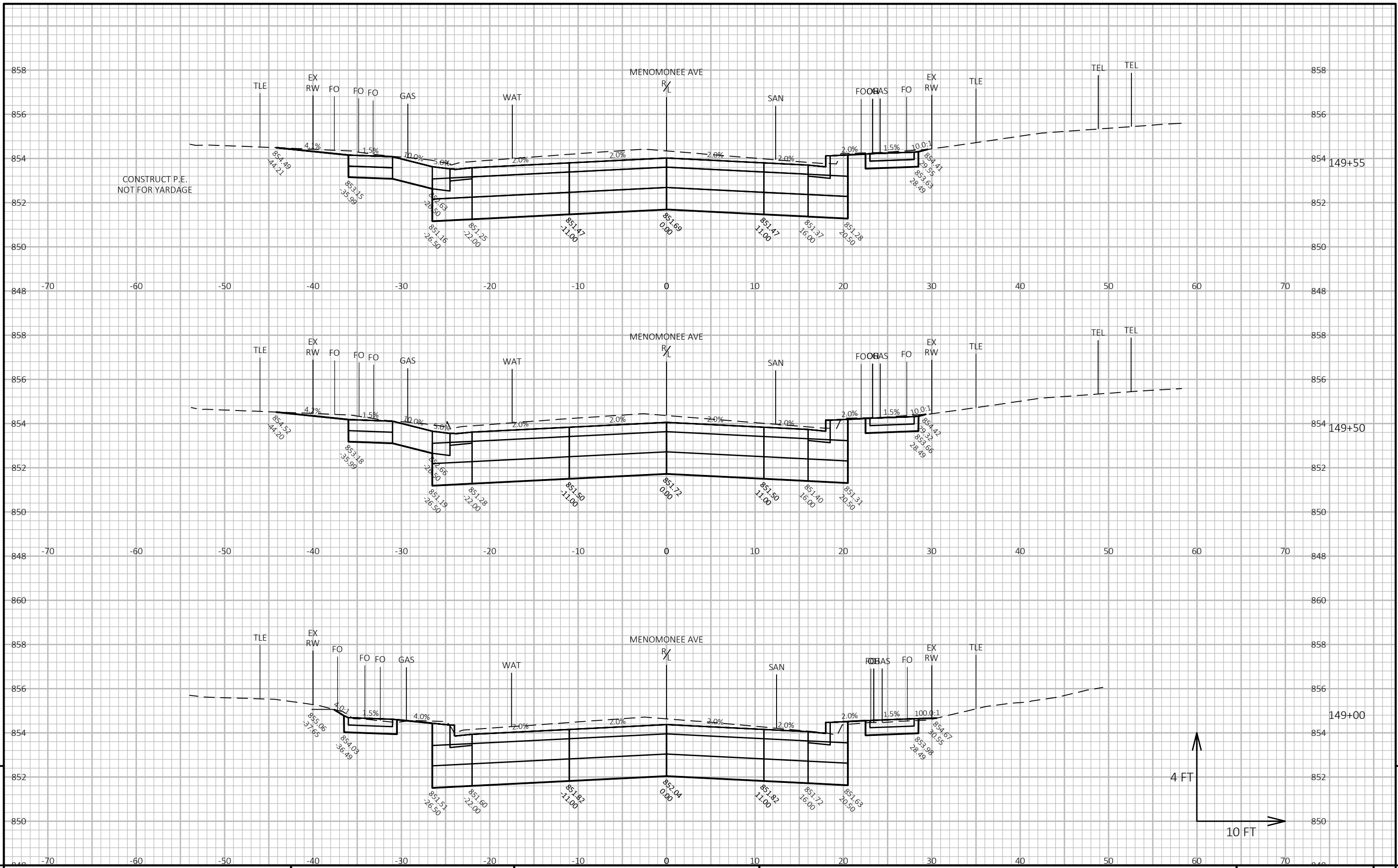
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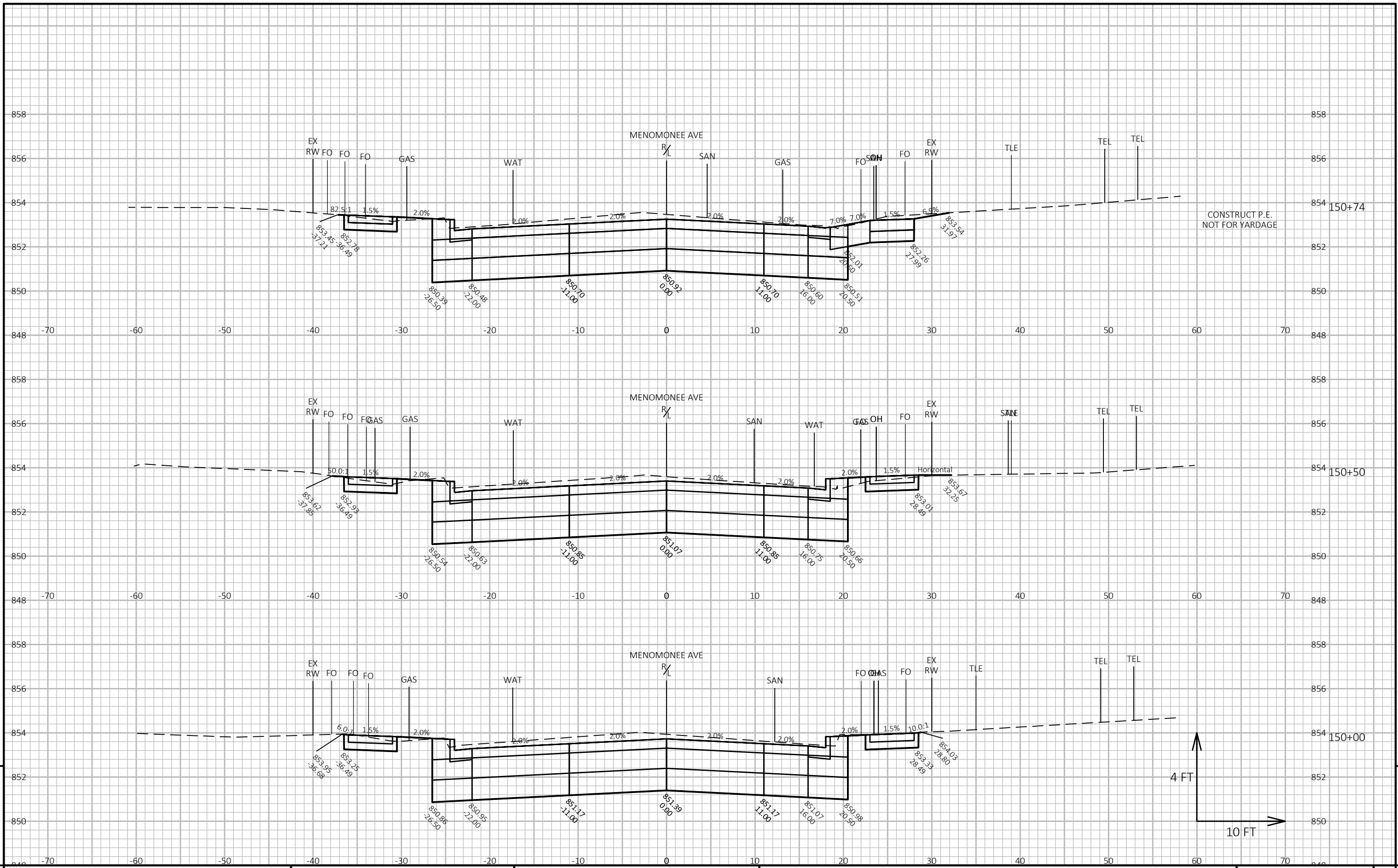
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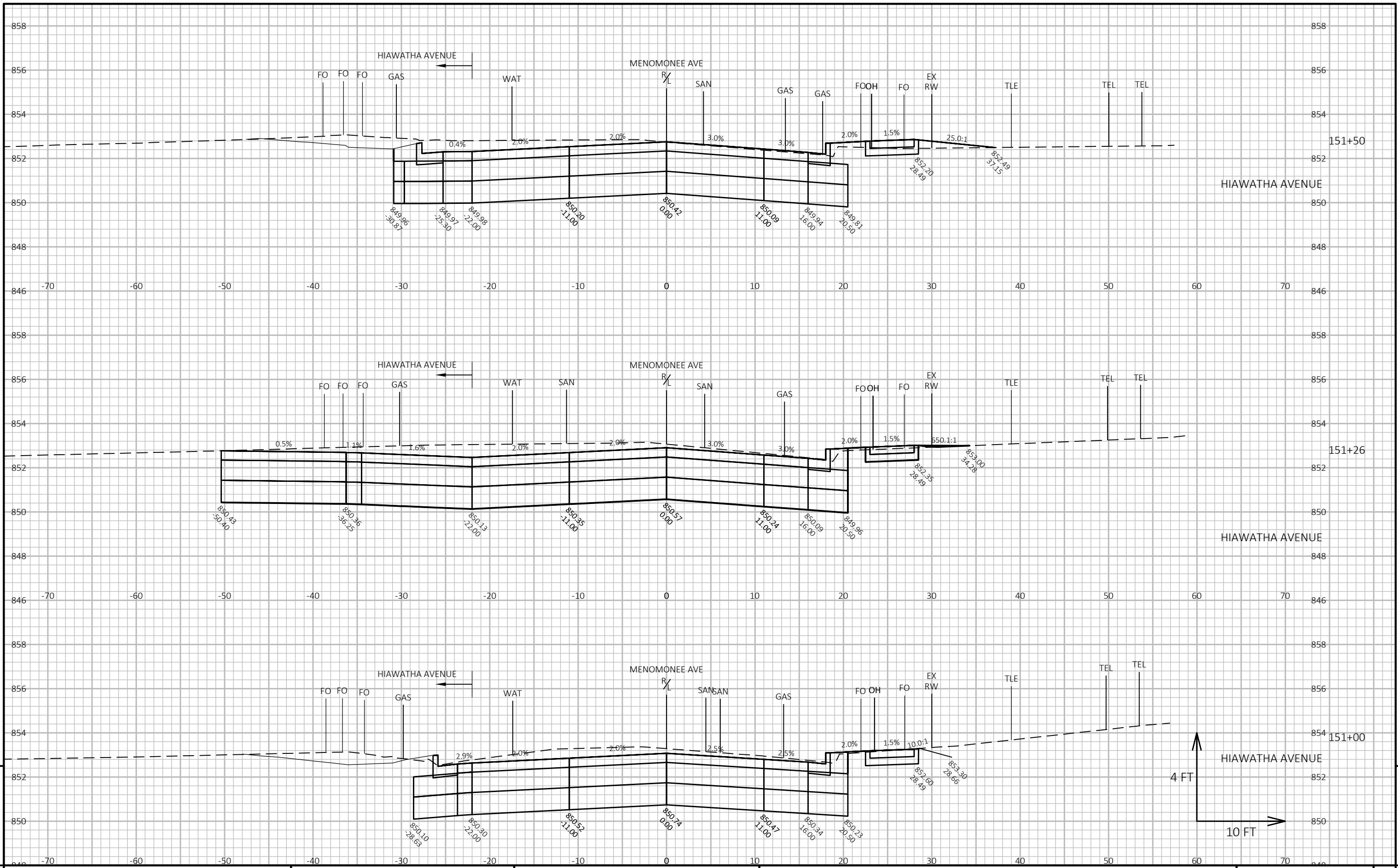
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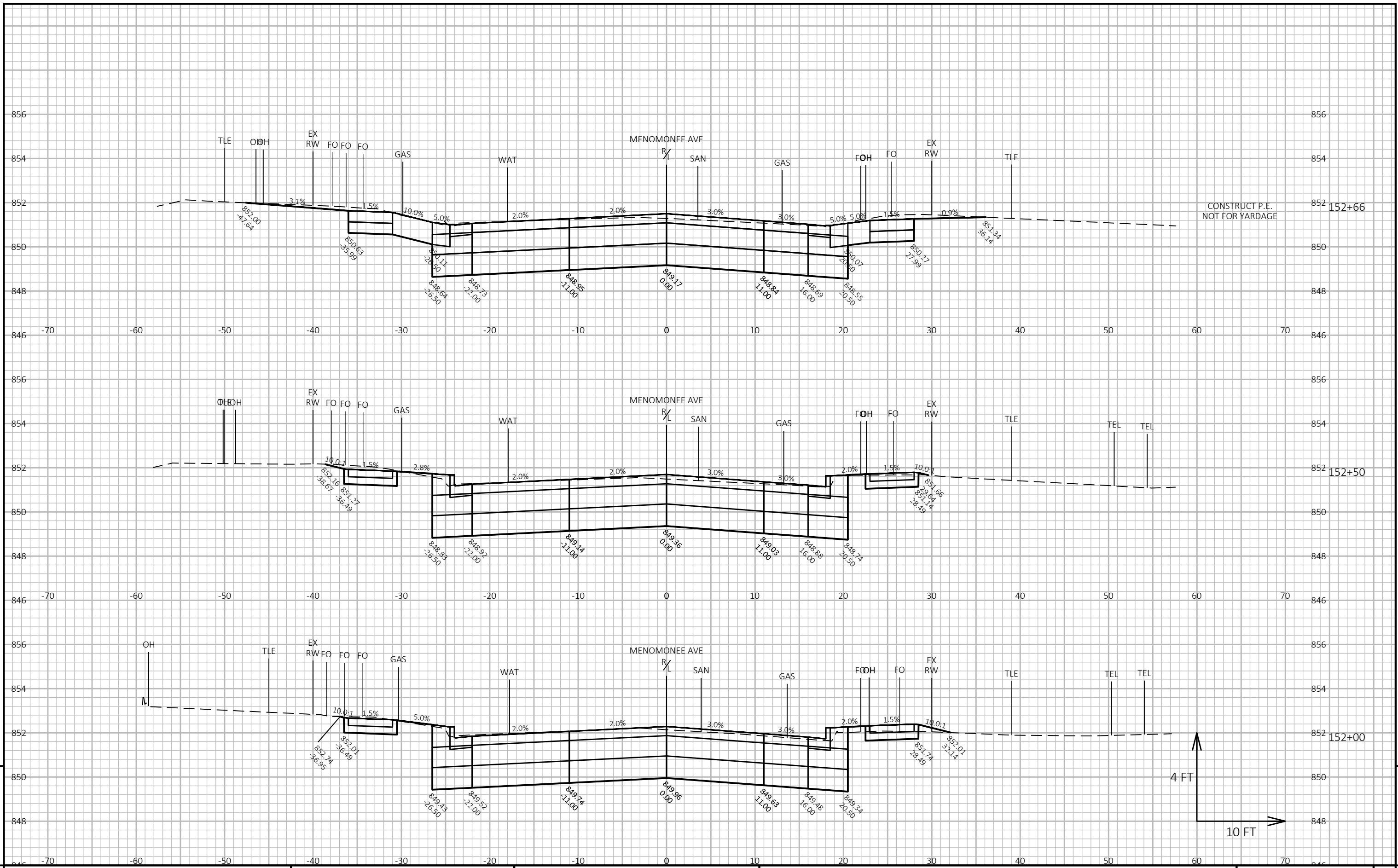
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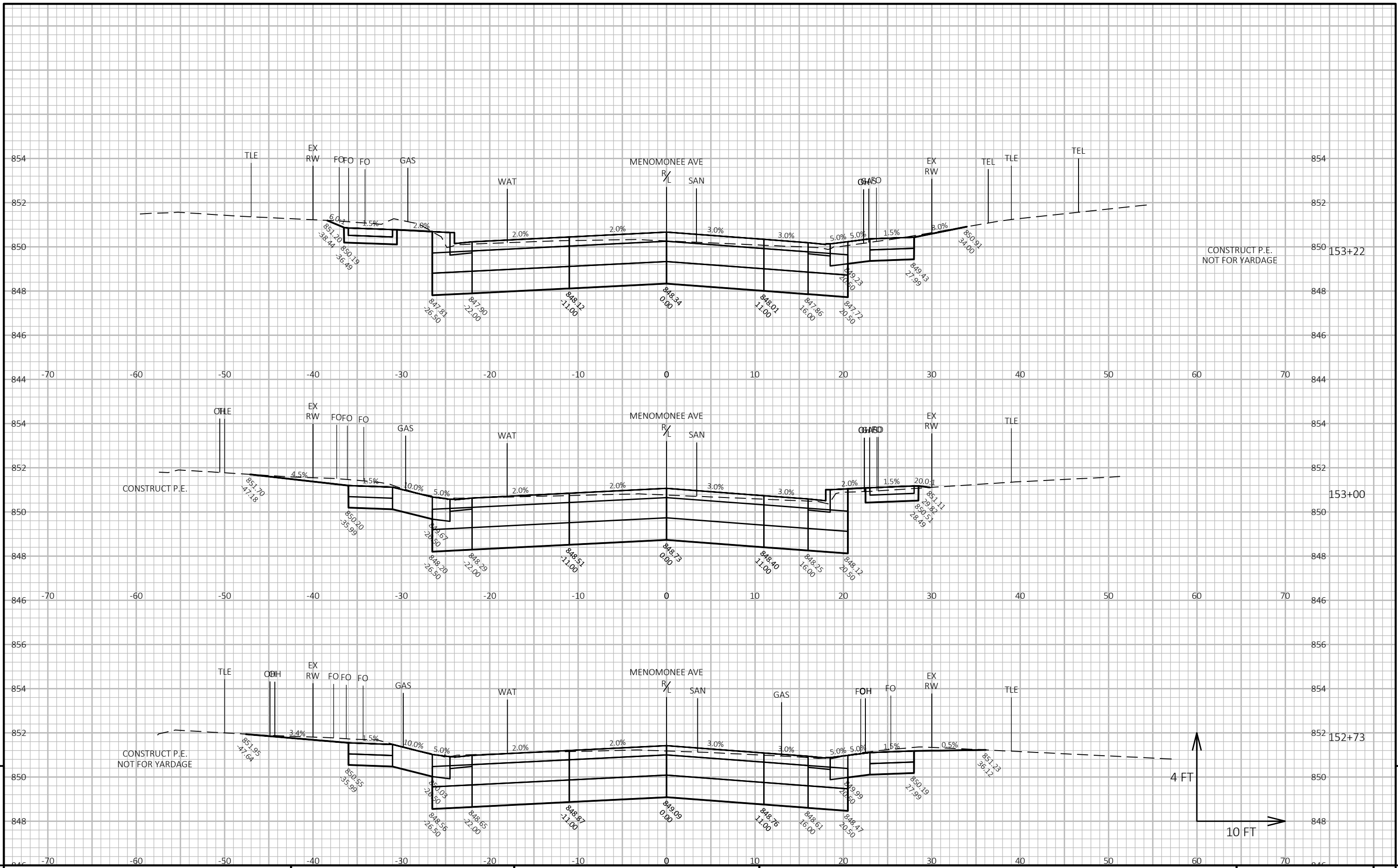
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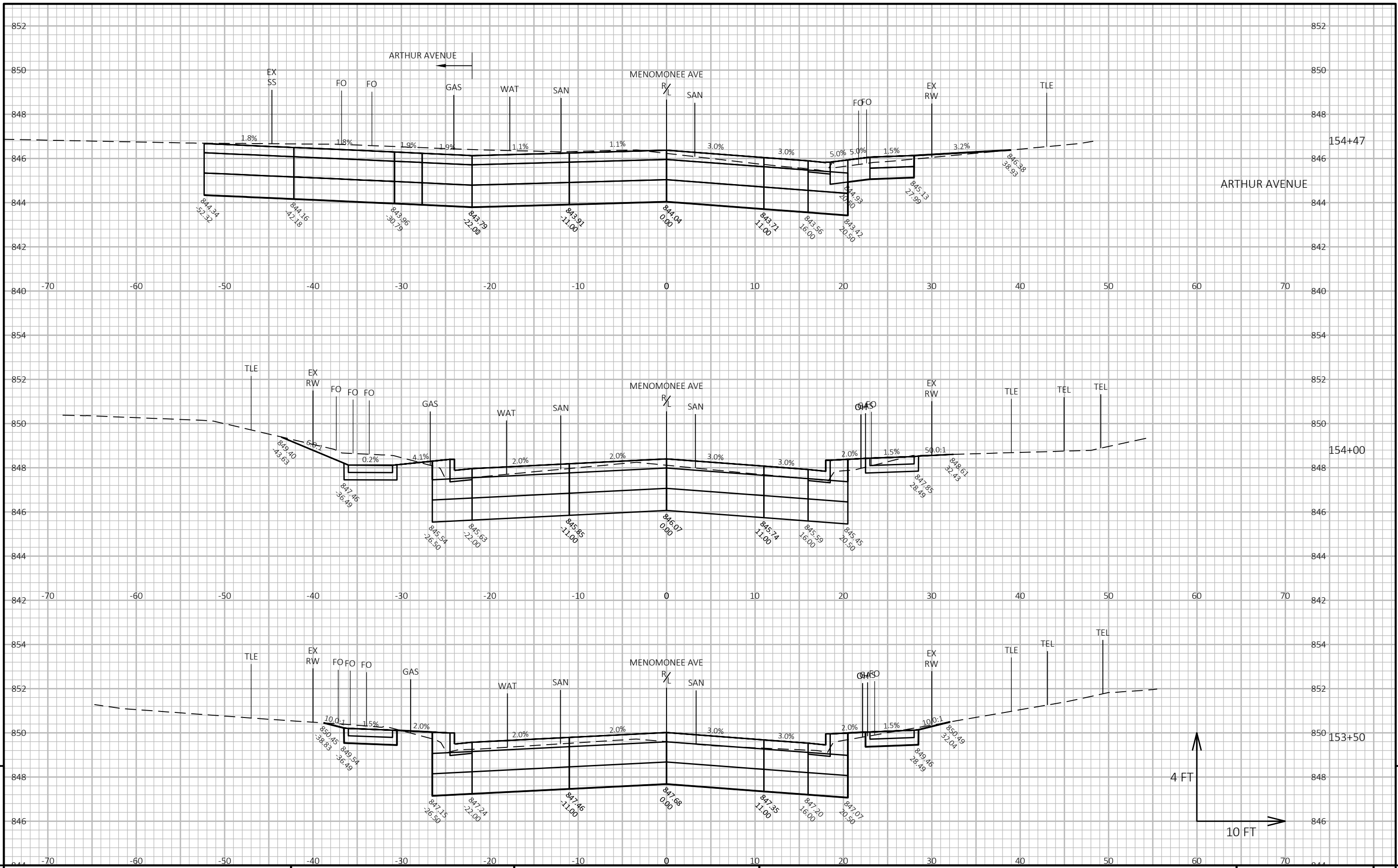
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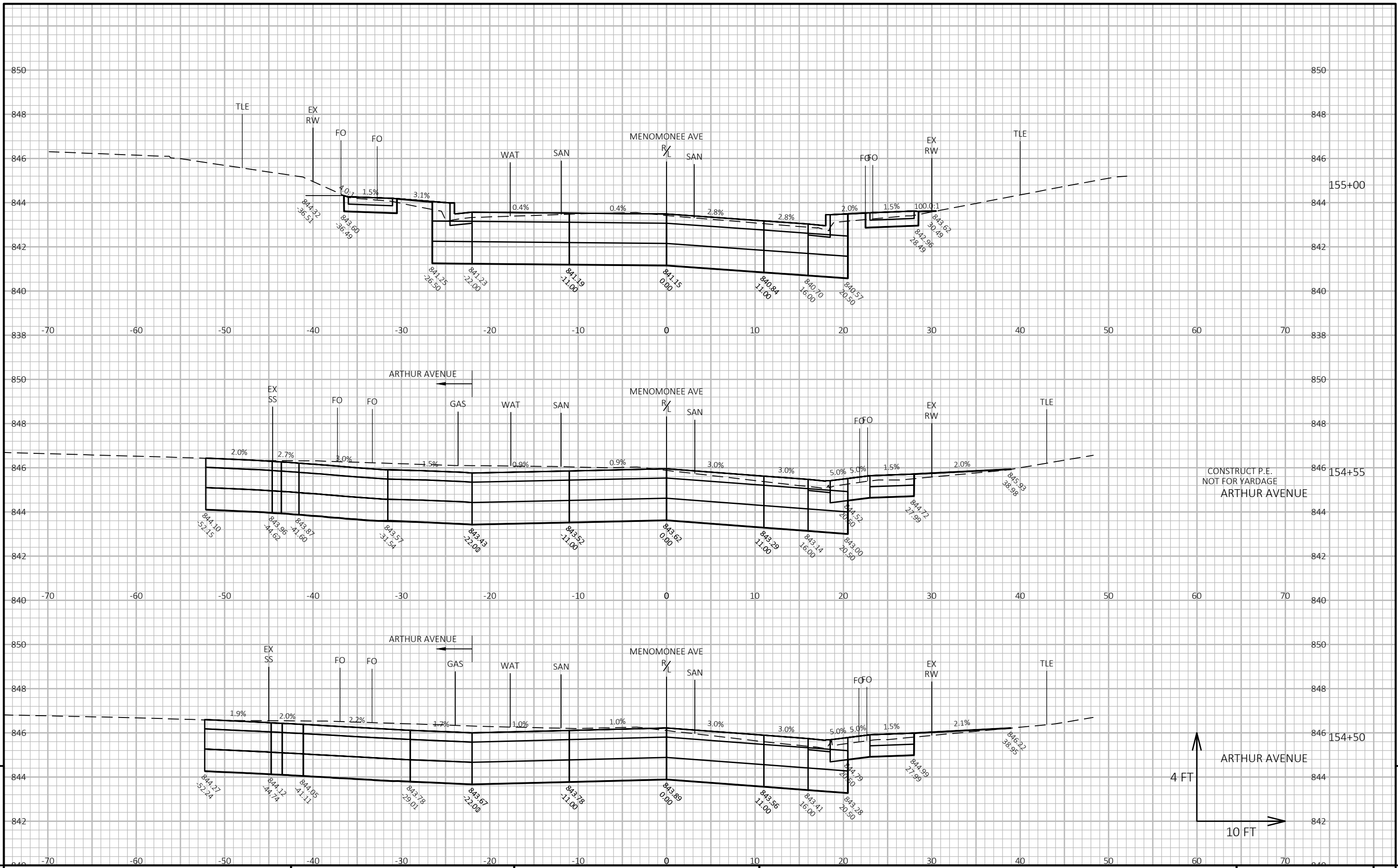
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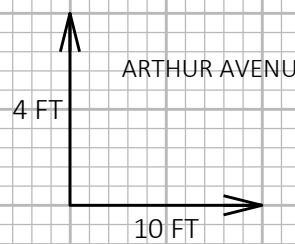
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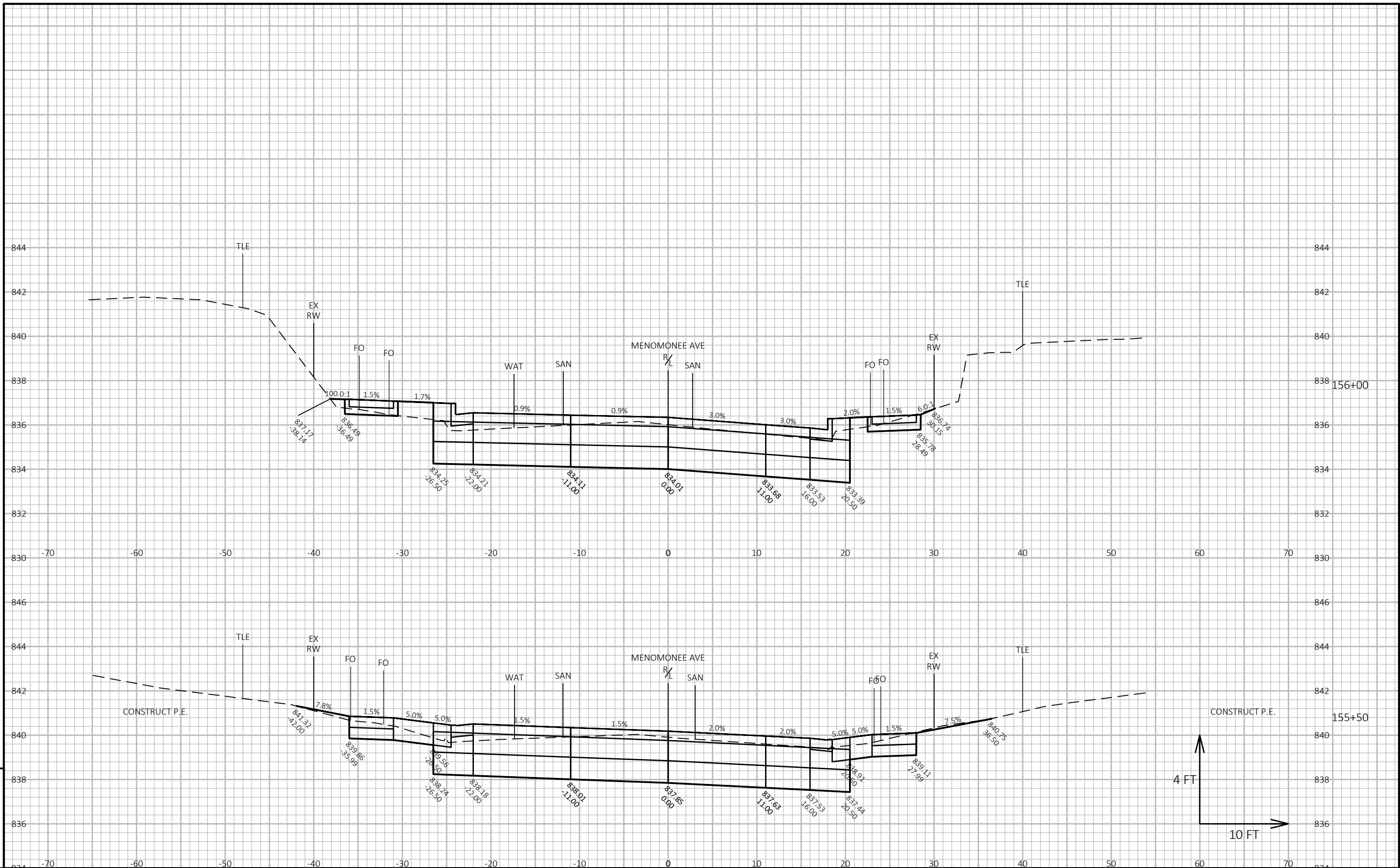
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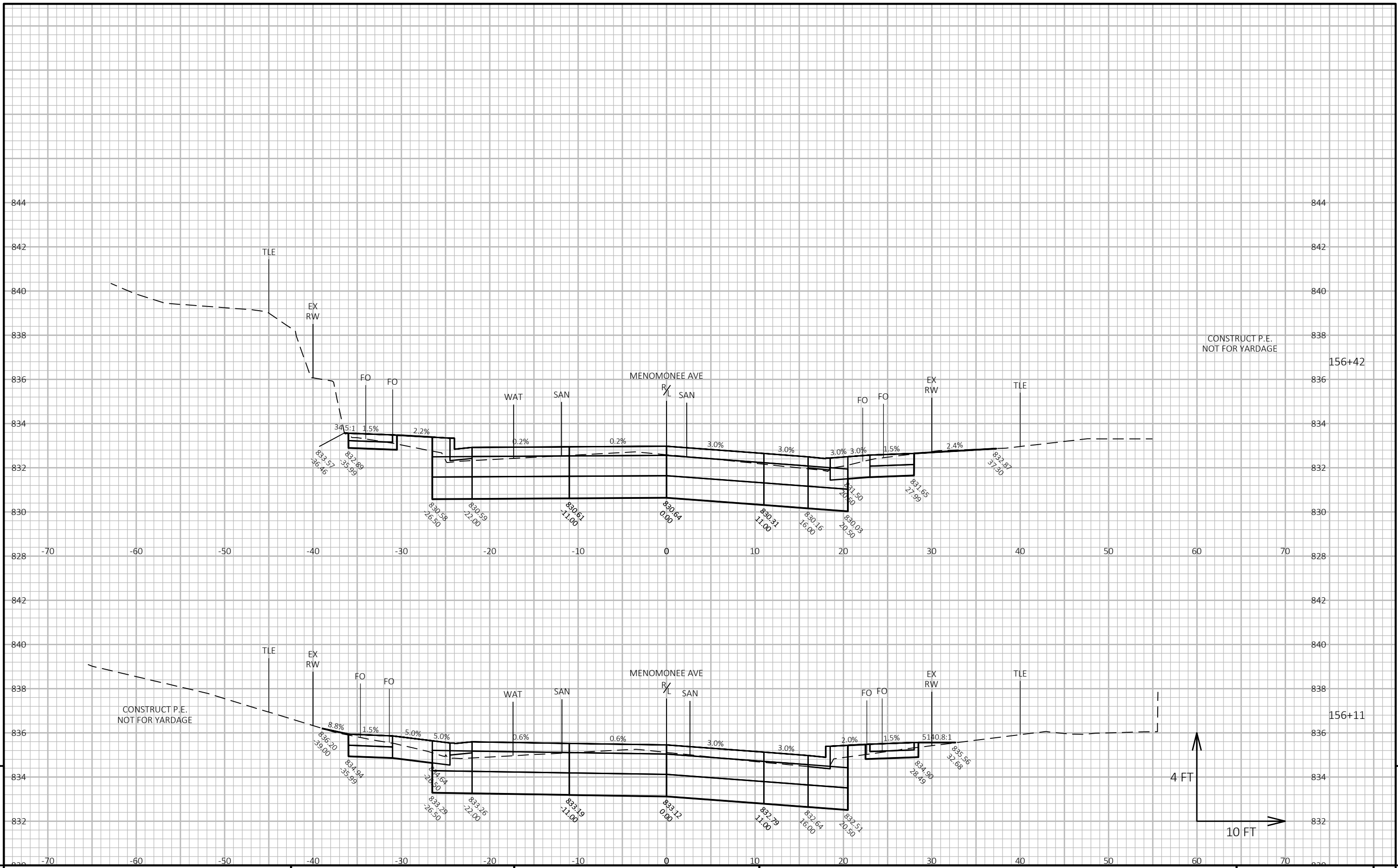
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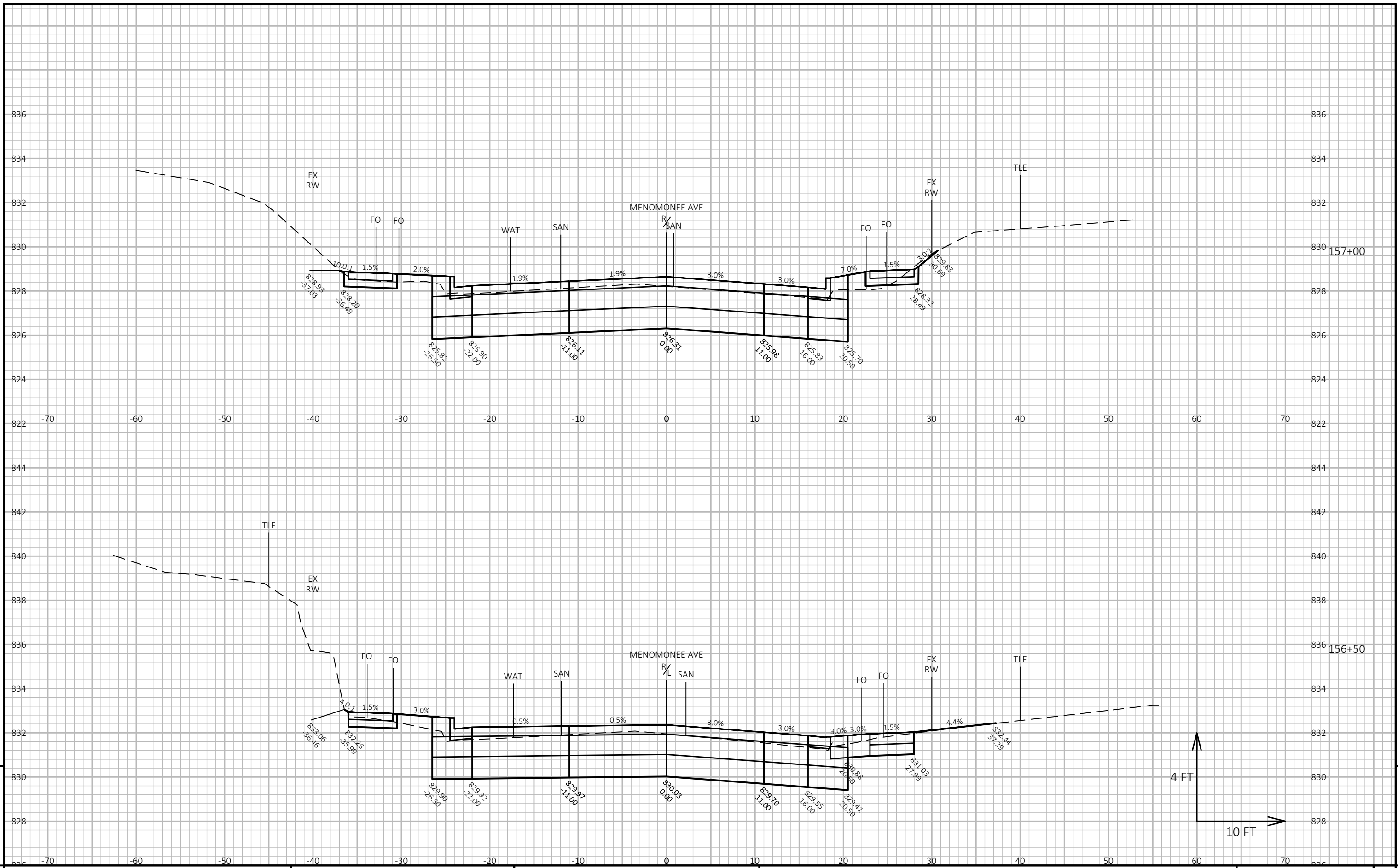
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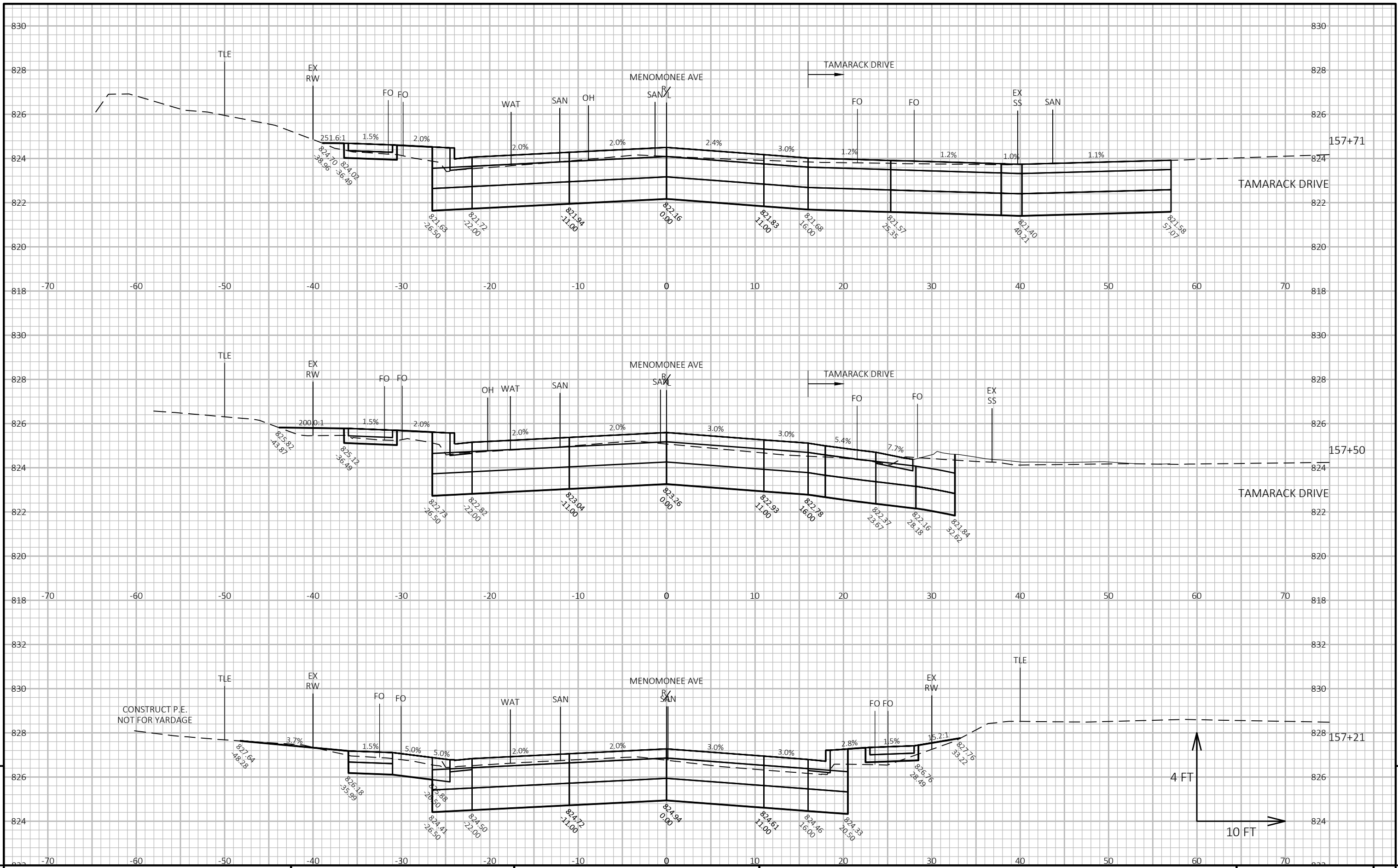
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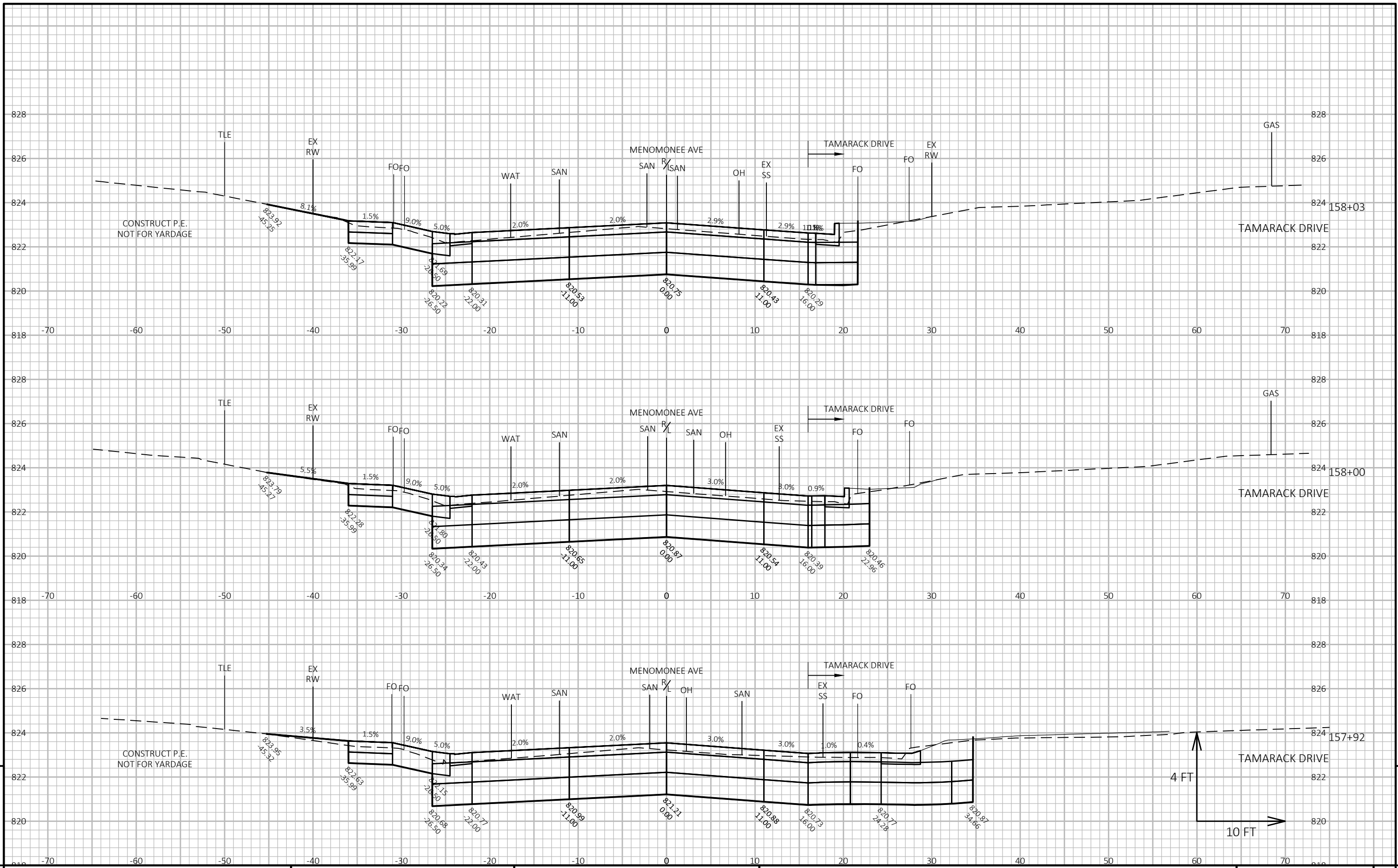
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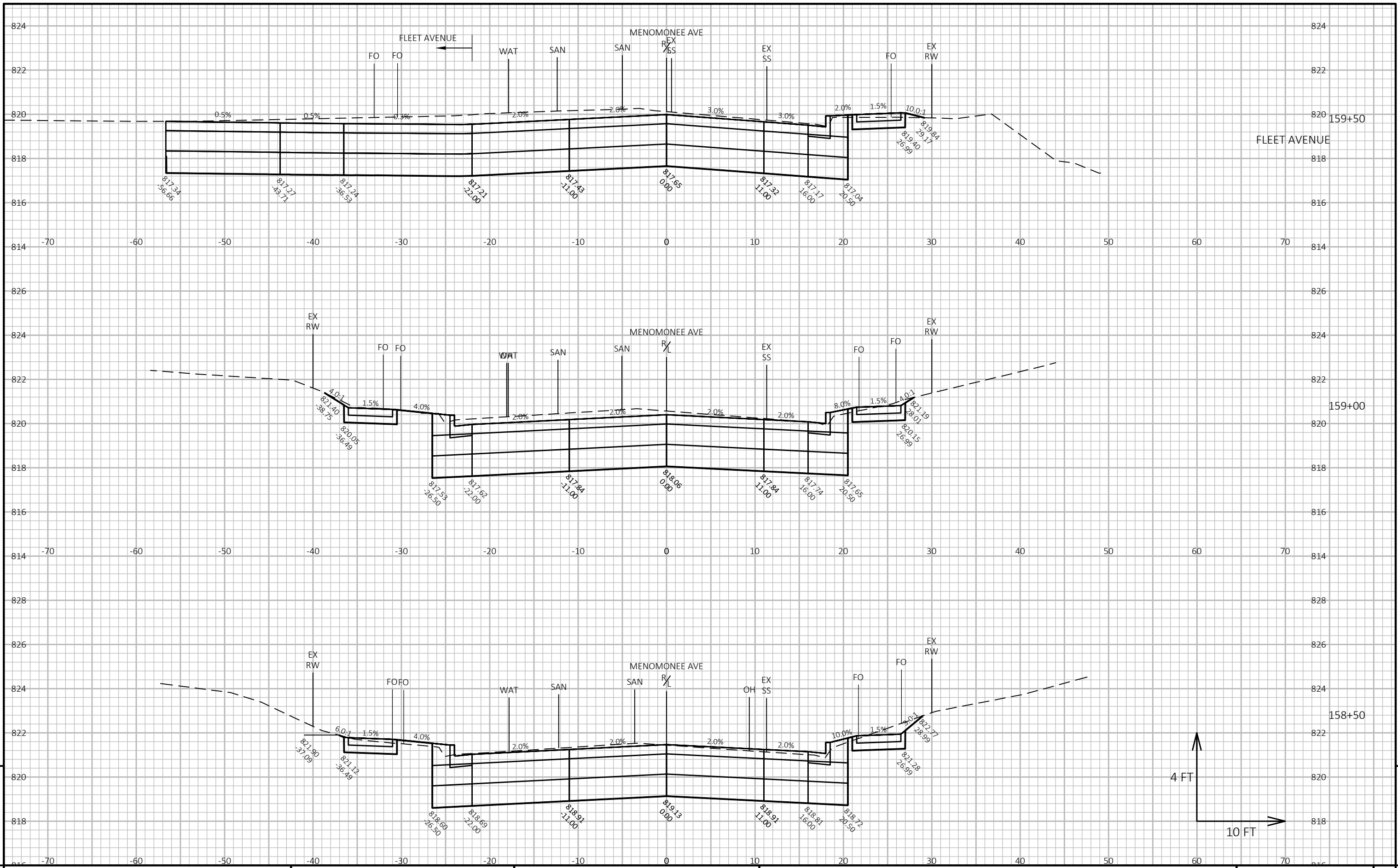
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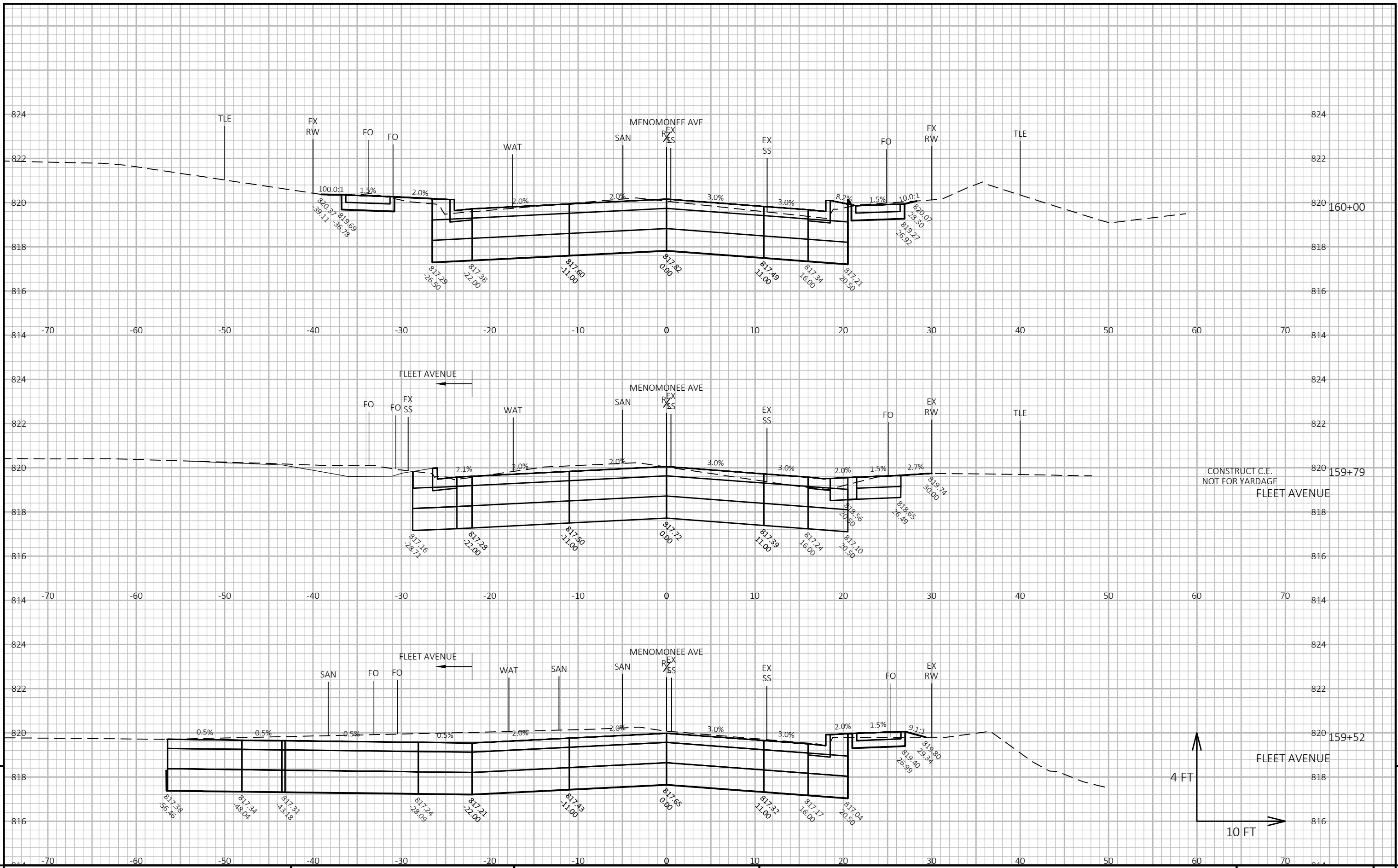
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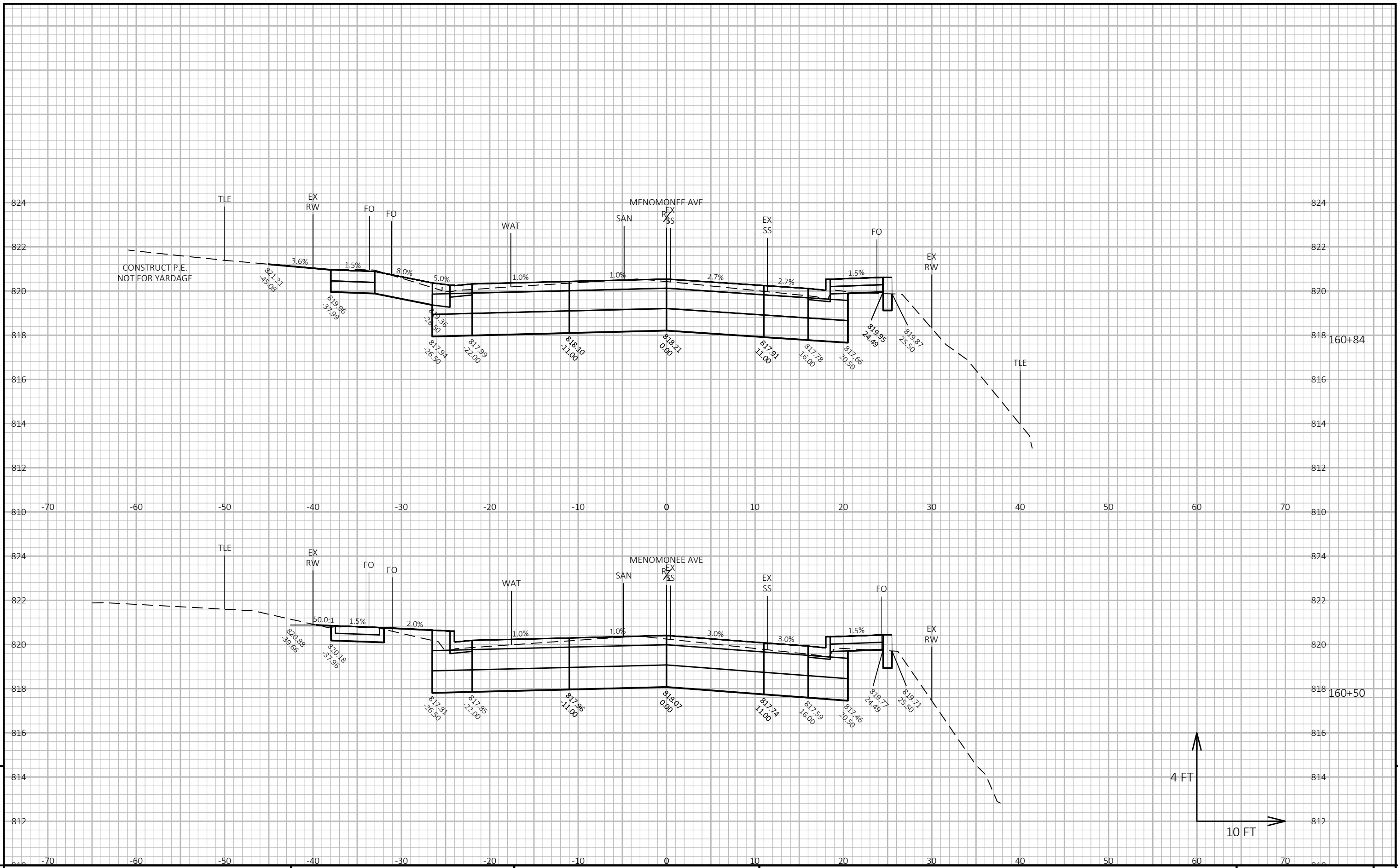
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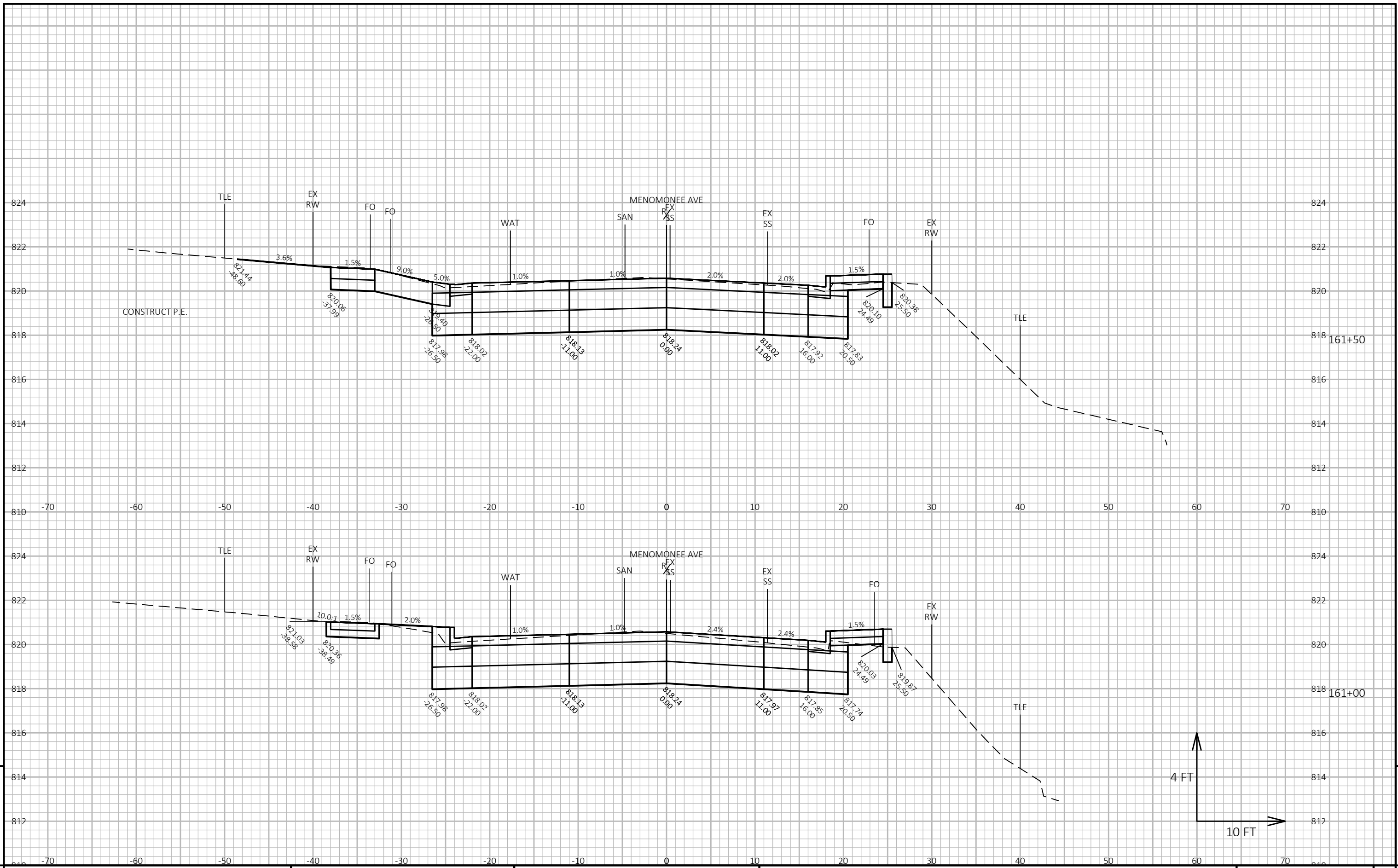
PROJECT NO: 2720-07-71 HWY: MENOMONEE AVE COUNTY: WAUKESHA CROSS SECTIONS: MENOMONEE AVE SHEET 9



PROJECT NO: 2720-07-71 HWY: MENOMONEE AVE COUNTY: WAUKESHA CROSS SECTIONS: MENOMONEE AVE SHEET 9

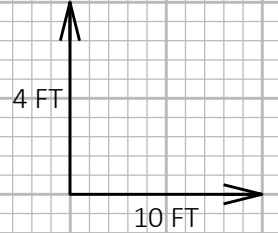


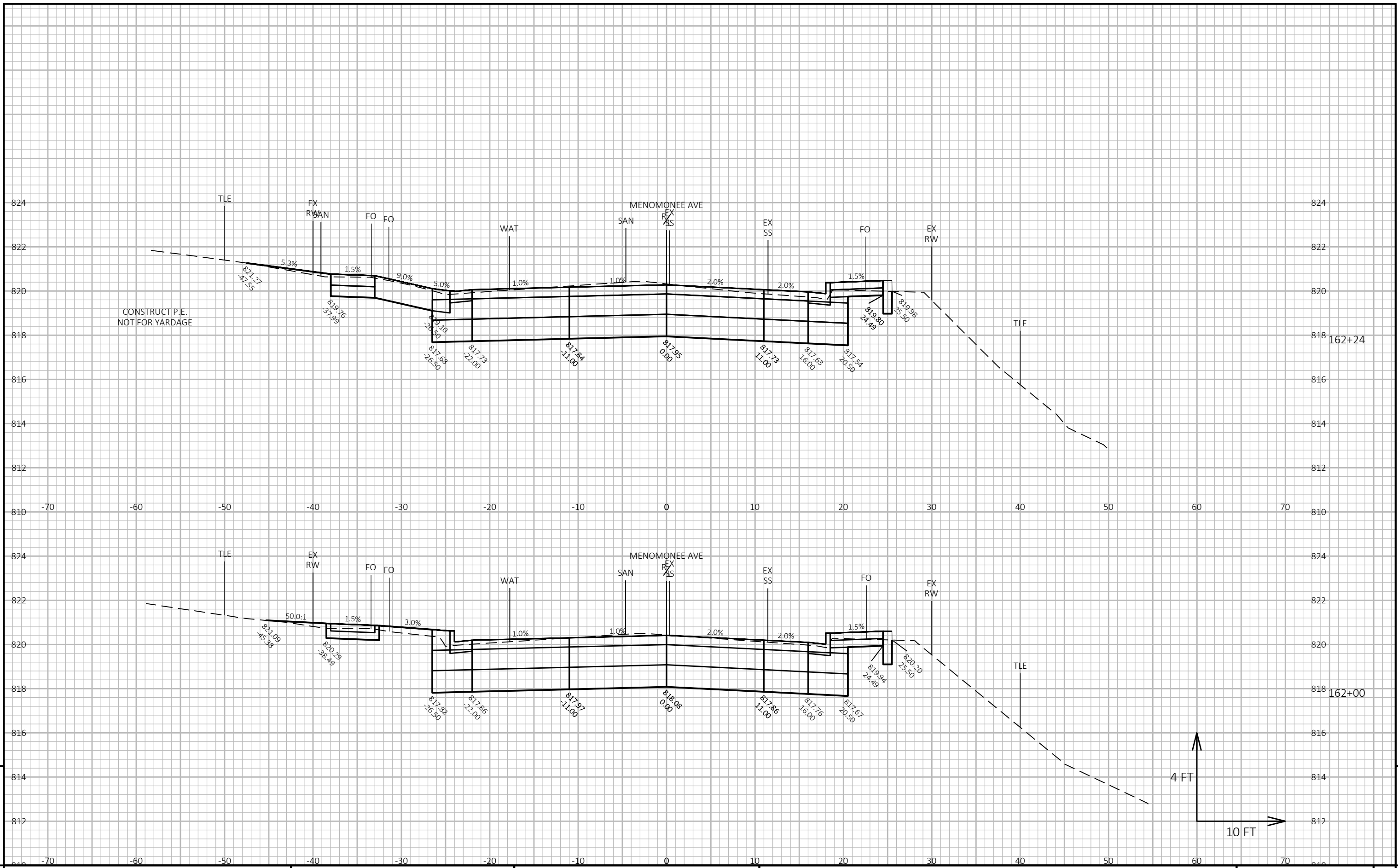
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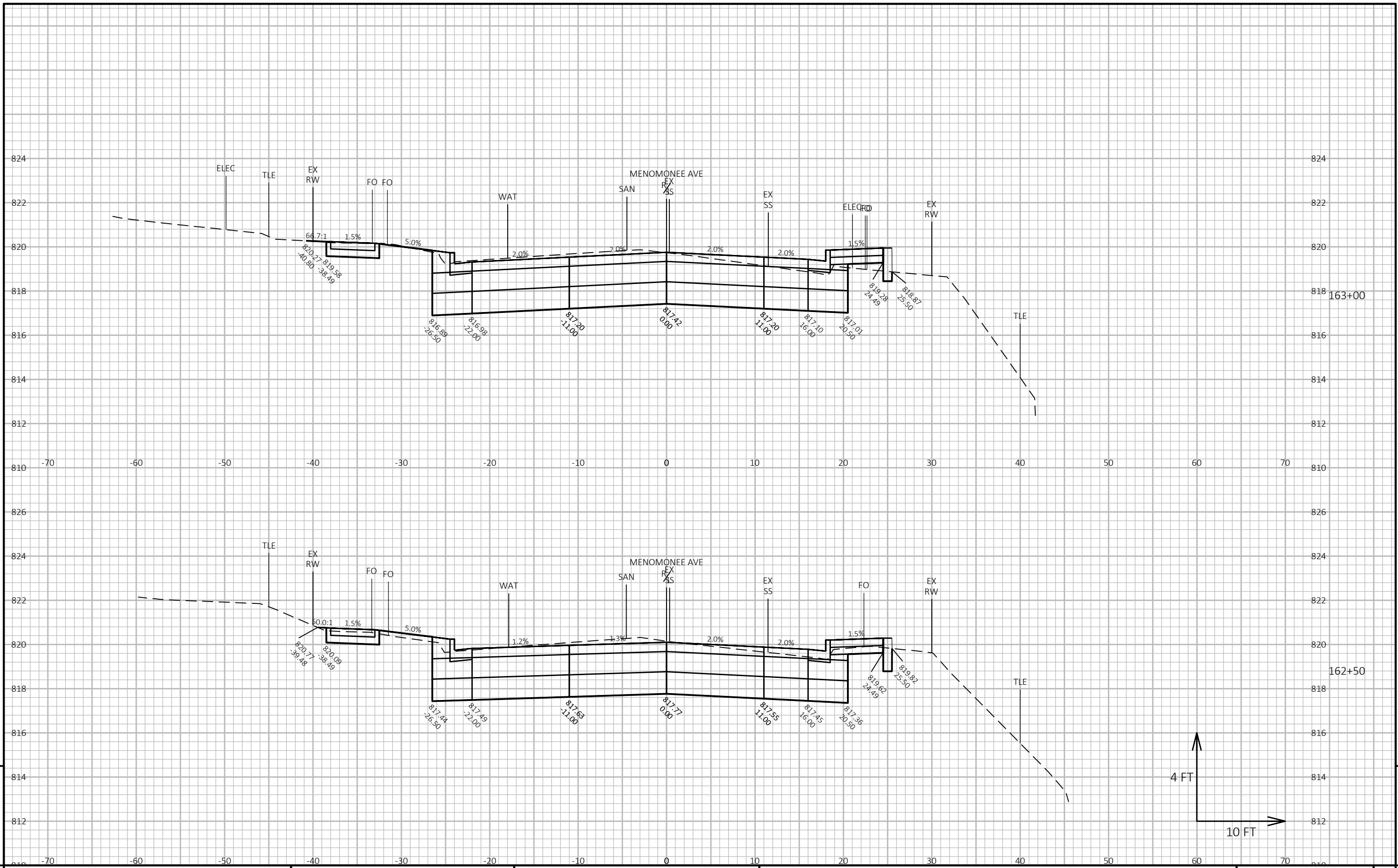
PROJECT NO: 2720-07-71 HWY: MENOMONEE AVE COUNTY: WAUKESHA CROSS SECTIONS: MENOMONEE AVE SHEET E

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PROJECT NO: 2720-07-71 HWY: MENOMONEE AVE COUNTY: WAUKESHA CROSS SECTIONS: MENOMONEE AVE SHEET 9



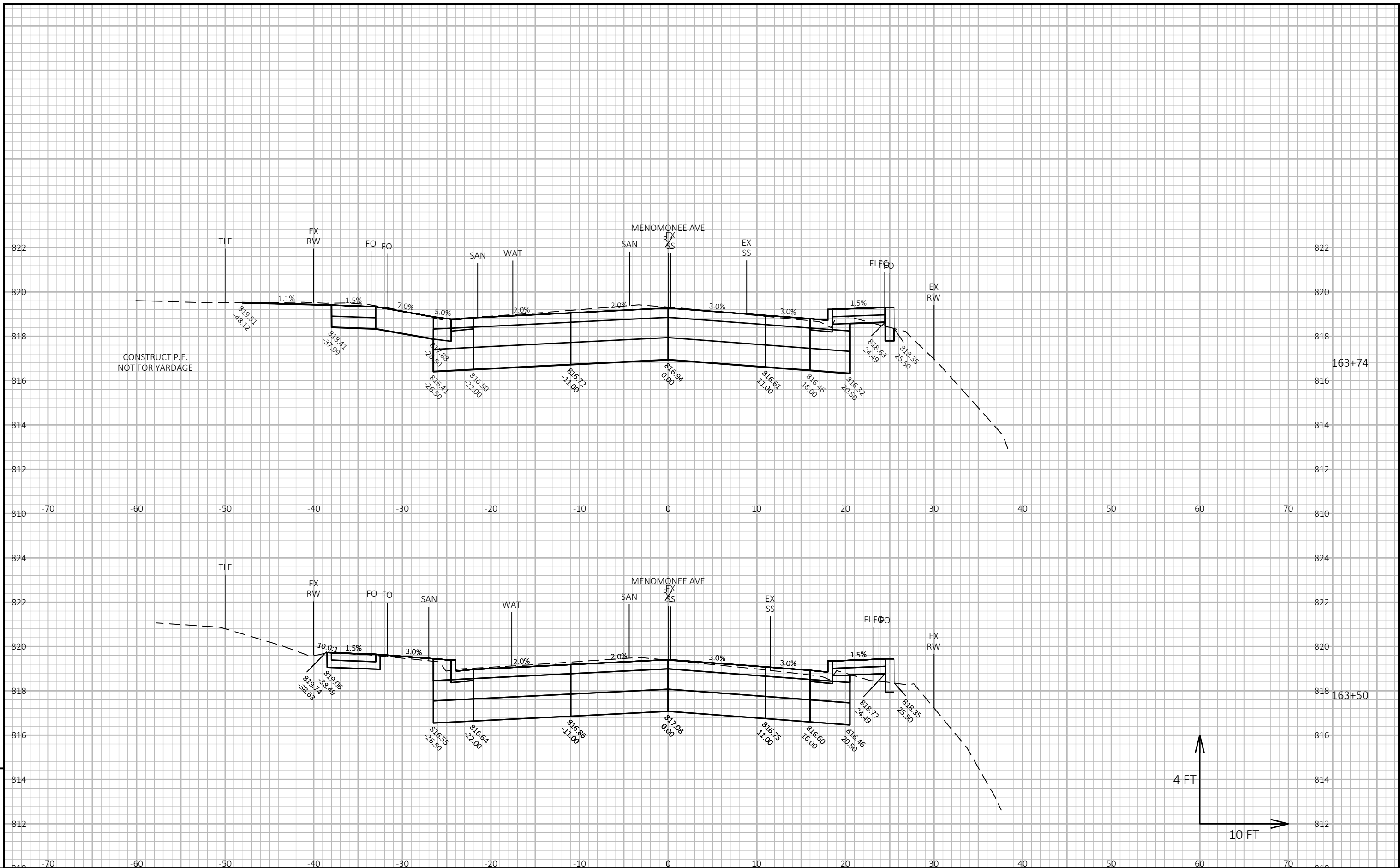
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PROJECT NO: 2720-07-71 HWY: MENOMONEE AVE COUNTY: WAUKESHA CROSS SECTIONS: MENOMONEE AVE SHEET E

FILE NAME: \\RSMITH.COM\BROOKFIELD\TRANSPORTATION\1222708\CIVIL3D\27200701\SHEETS\090203-XS.DWG PLOT DATE: 9/17/2024 11:22 AM PLOT BY: JOHNSON, BRENDEN PLOT NAME: PLOT SCALE: 1 IN:10 FT HORZ. / 1 IN:4 FT VERT. WISDOT/CADD SHEET 49

LAYOUT NAME - 72



PROJECT NO: 2720-07-71

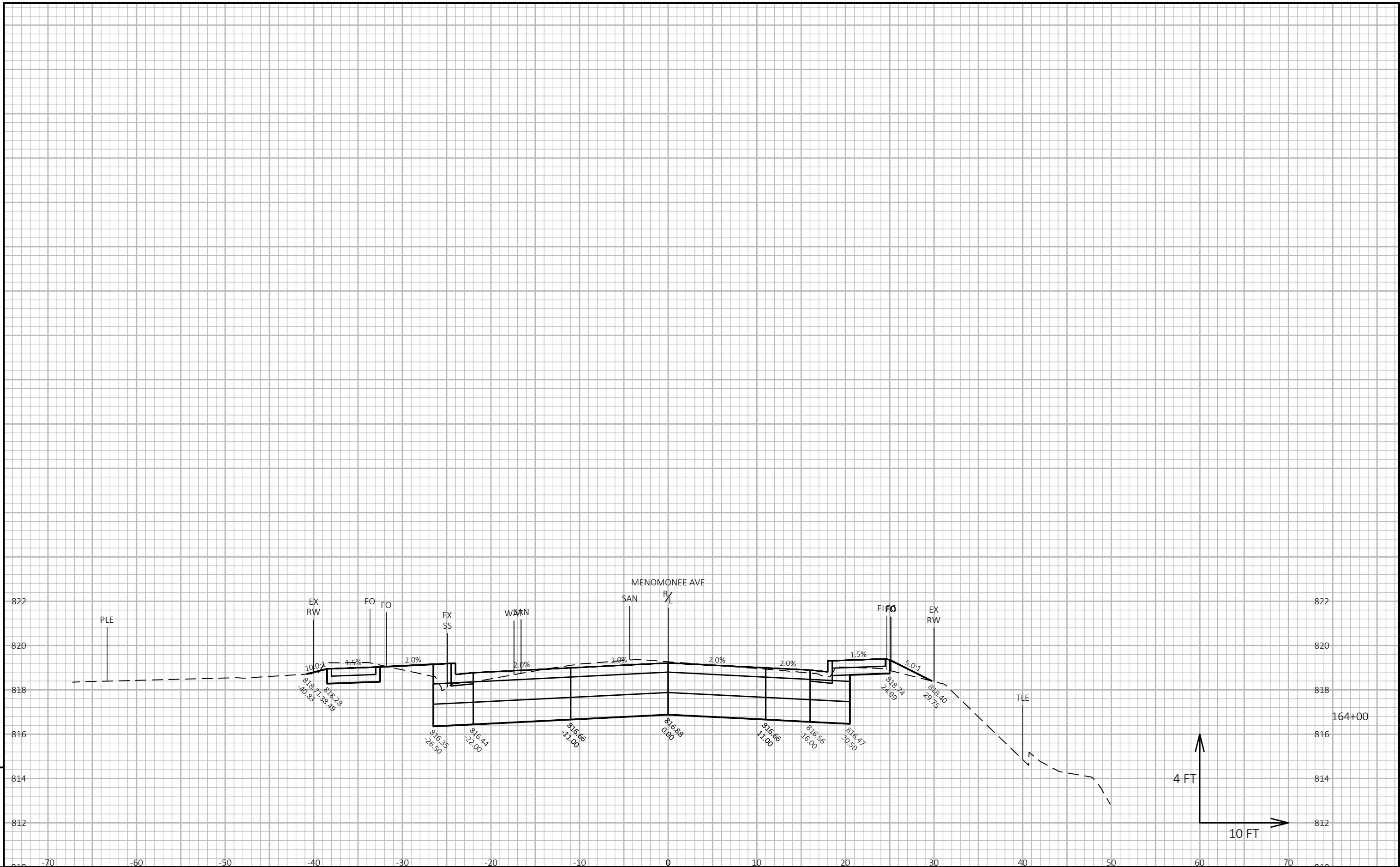
HWY: MENOMONEE AVE

COUNTY: WAUKESHA

CROSS SECTIONS: MENOMONEE AVE

SHEET

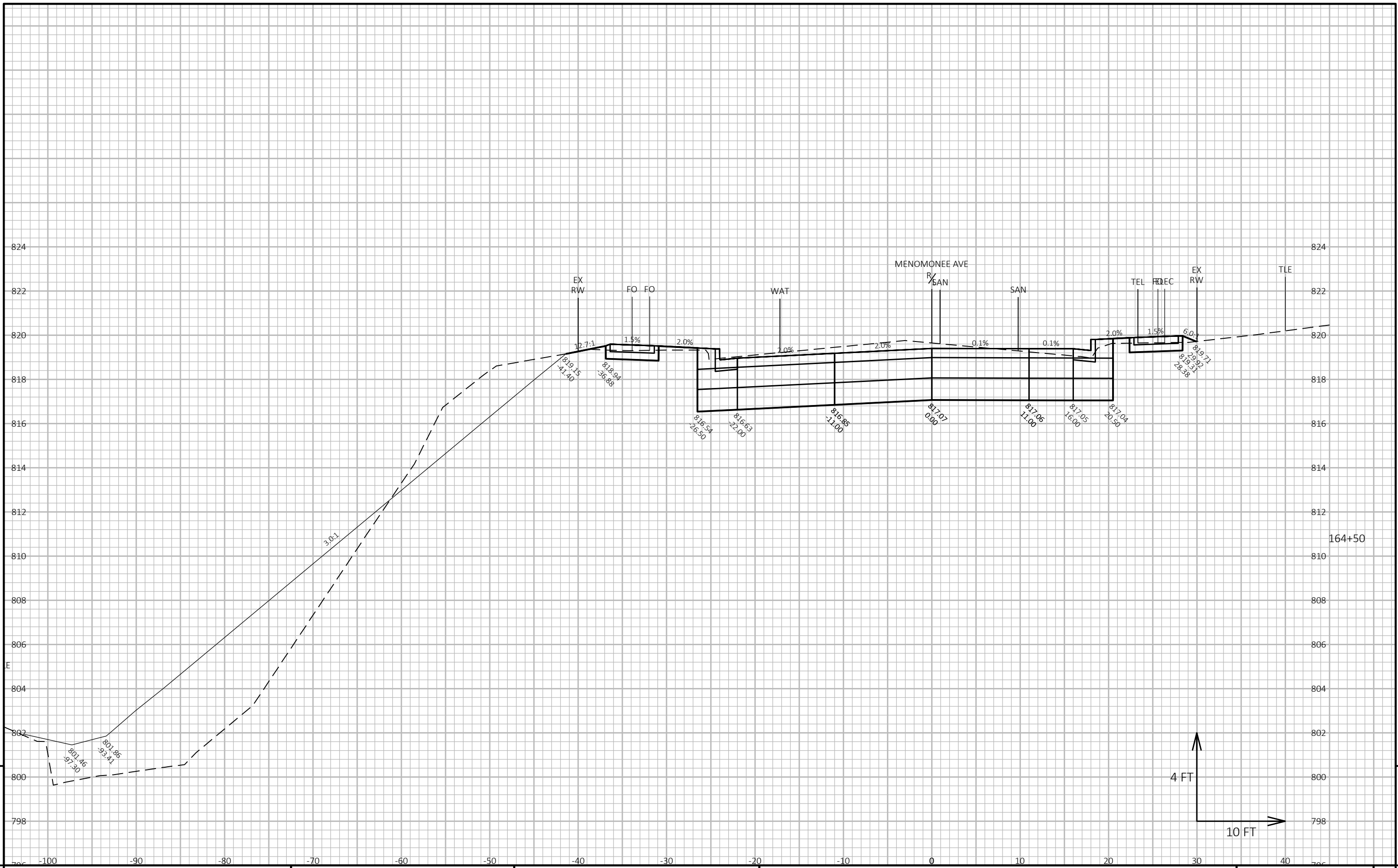
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PROJECT NO: 2720-07-71 HWY: MENOMONEE AVE COUNTY: WAUKESHA CROSS SECTIONS: MENOMONEE AVE SHEET E



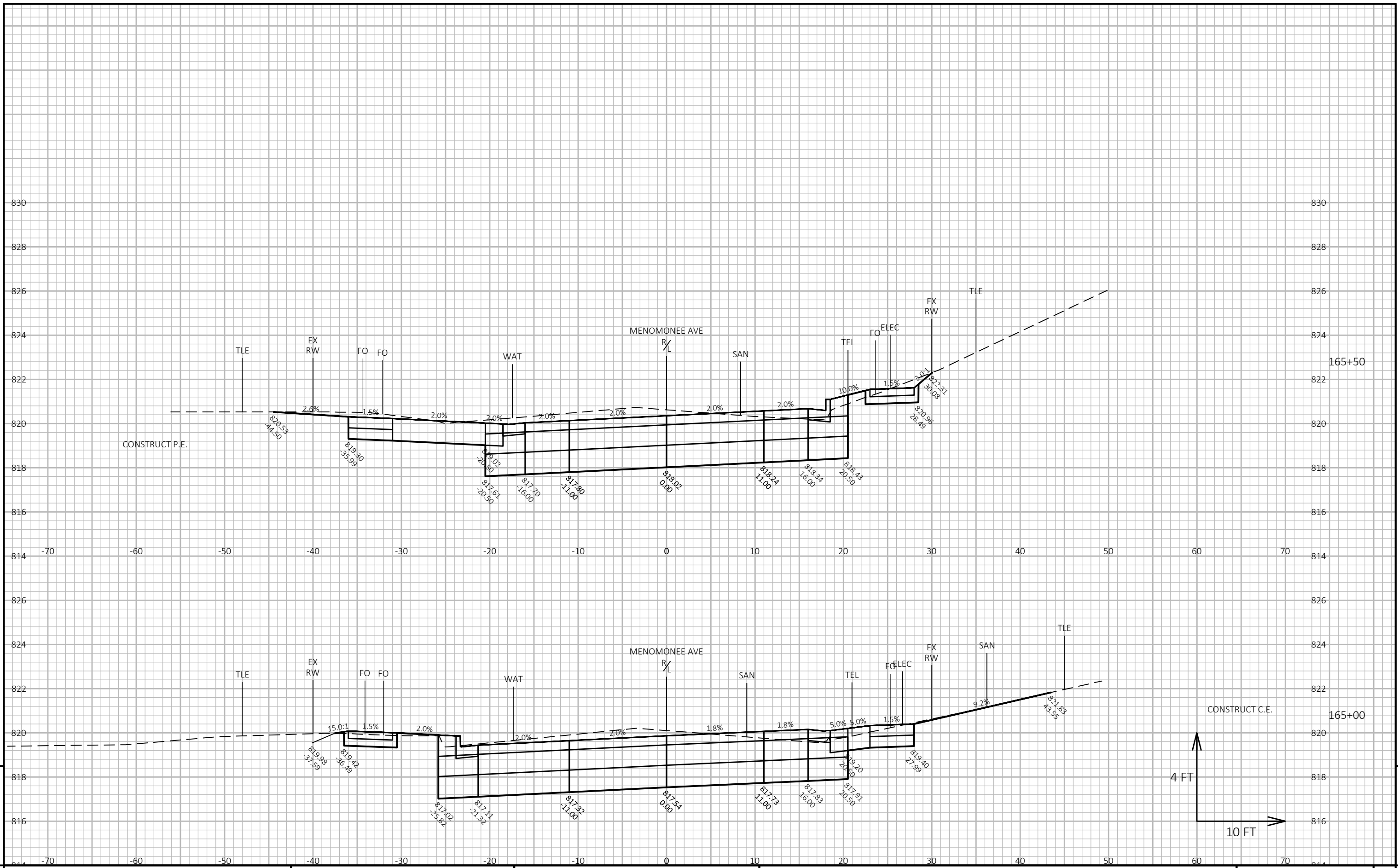
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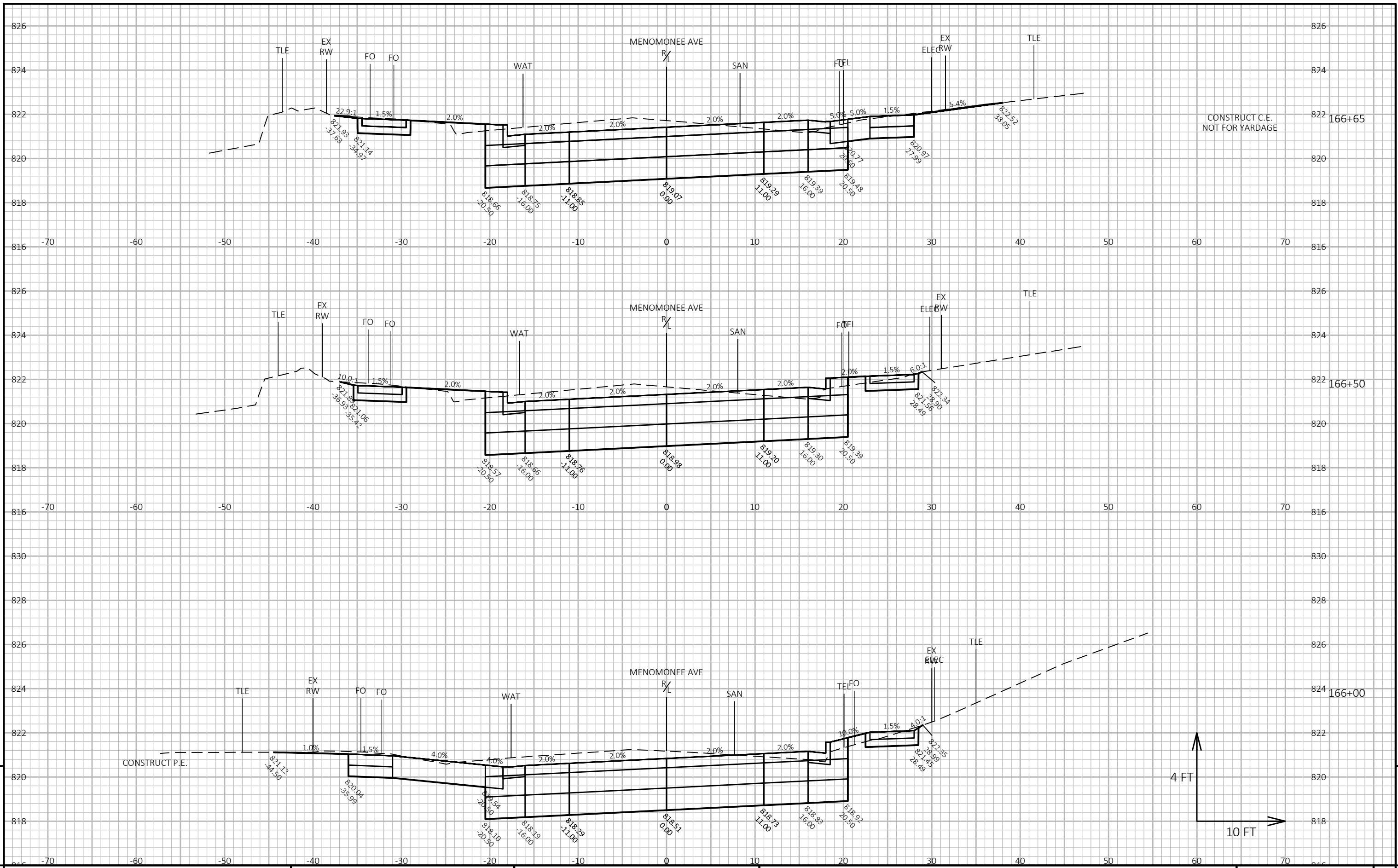
PROJECT NO: 2720-07-71 HWY: MENOMONEE AVE COUNTY: WAUKESHA CROSS SECTIONS: MENOMONEE AVE SHEET E

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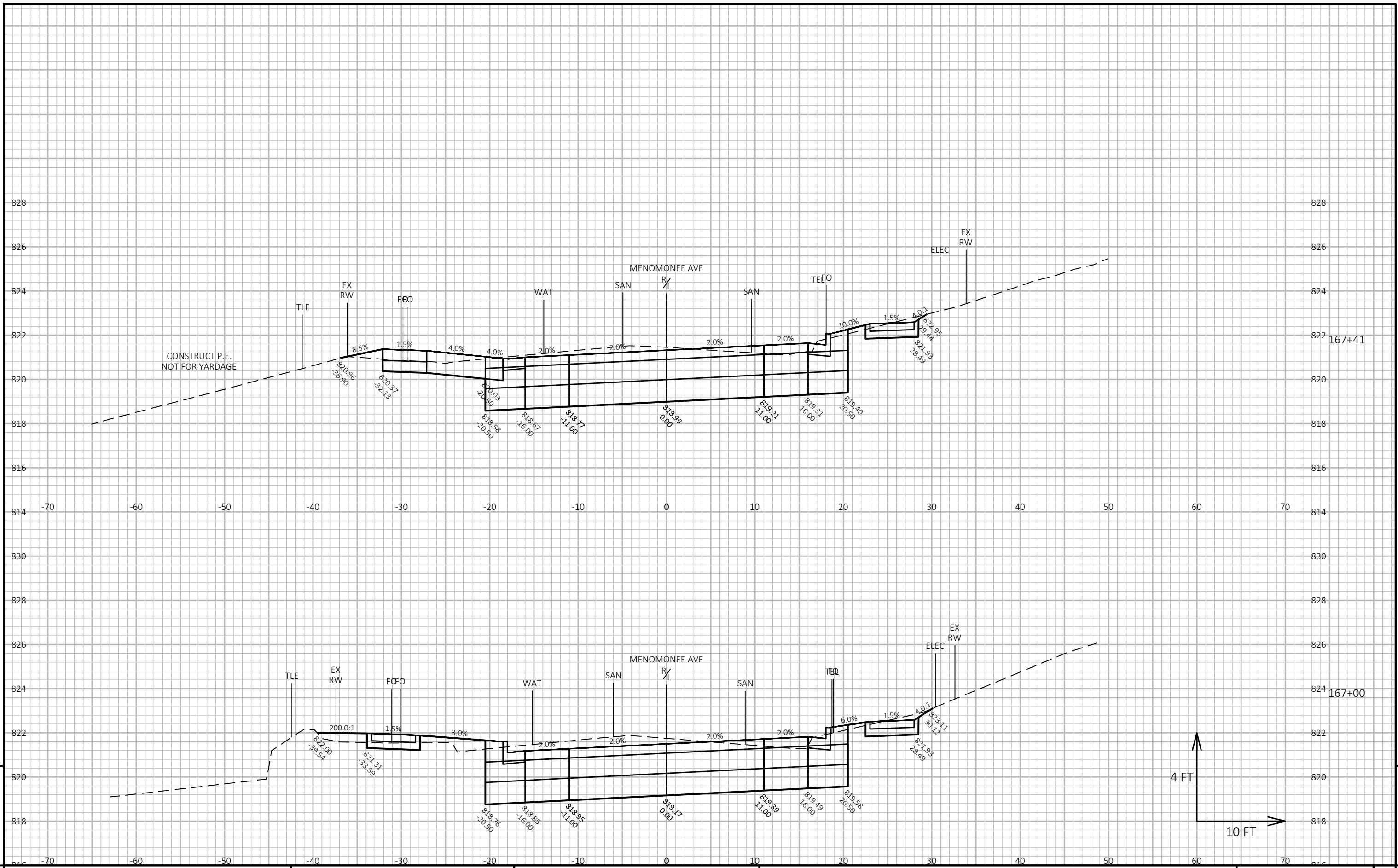
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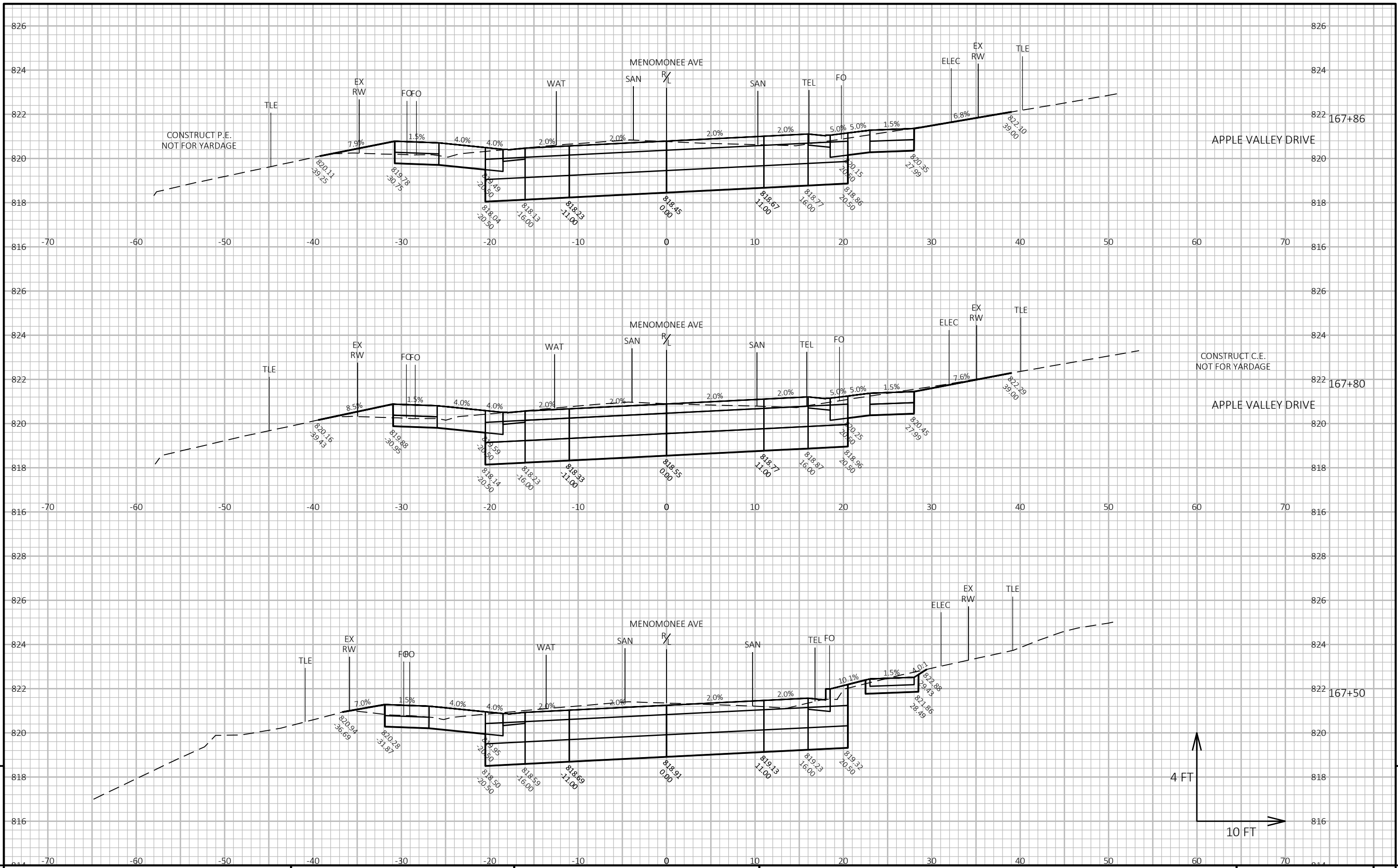
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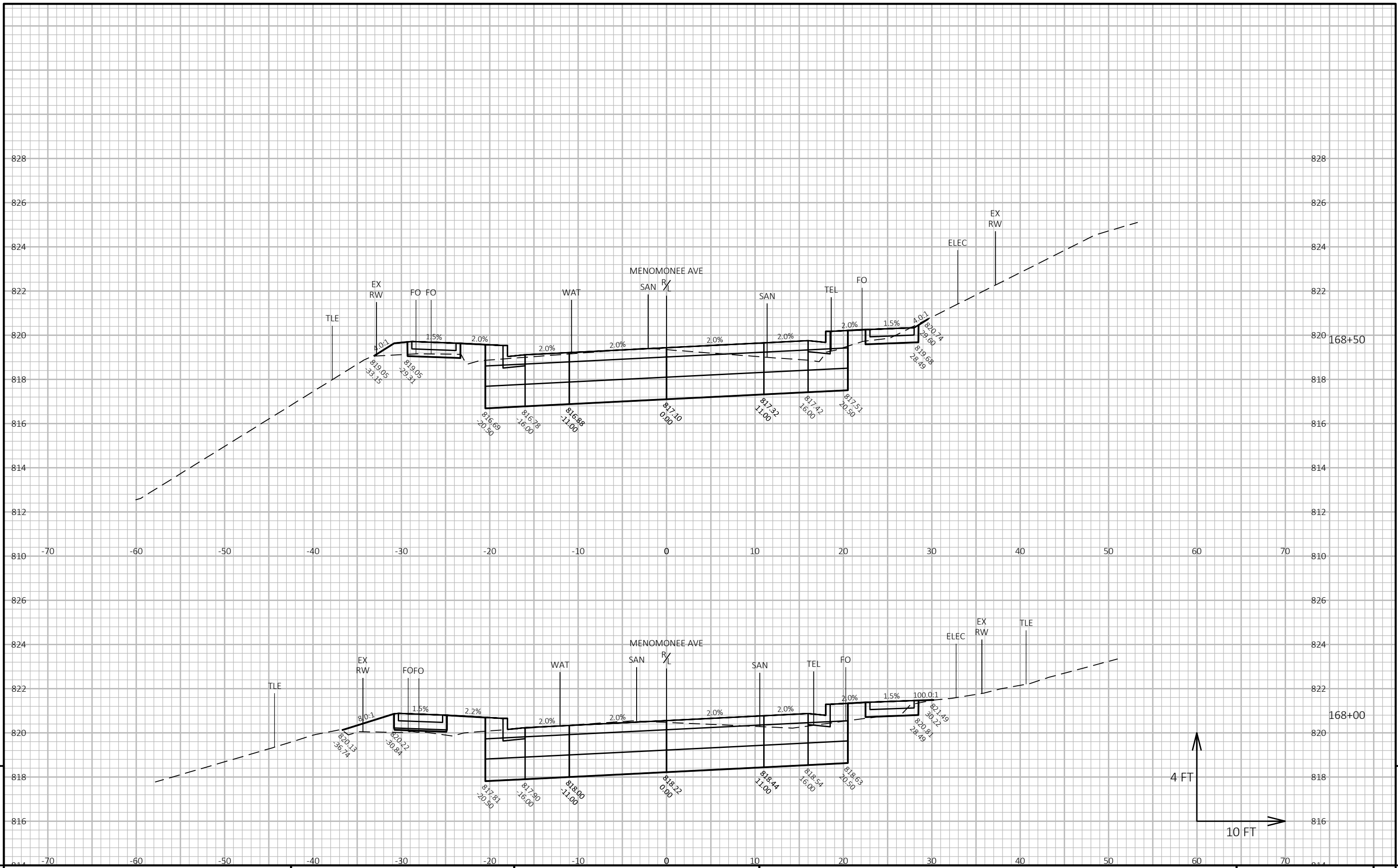
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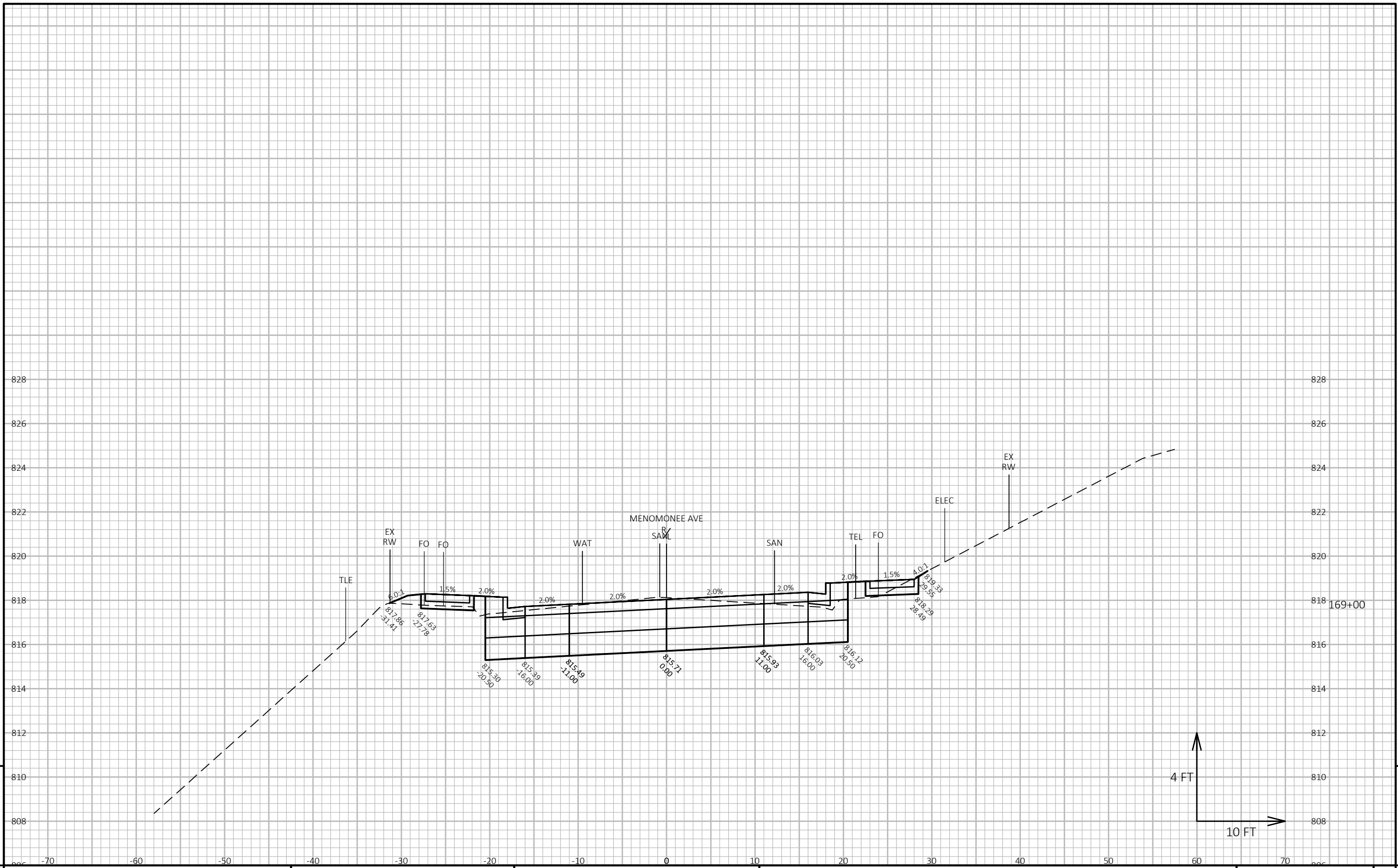
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PROJECT NO: 2720-07-71 HWY: MENOMONEE AVE COUNTY: WAUKESHA CROSS SECTIONS: MENOMONEE AVE SHEET E



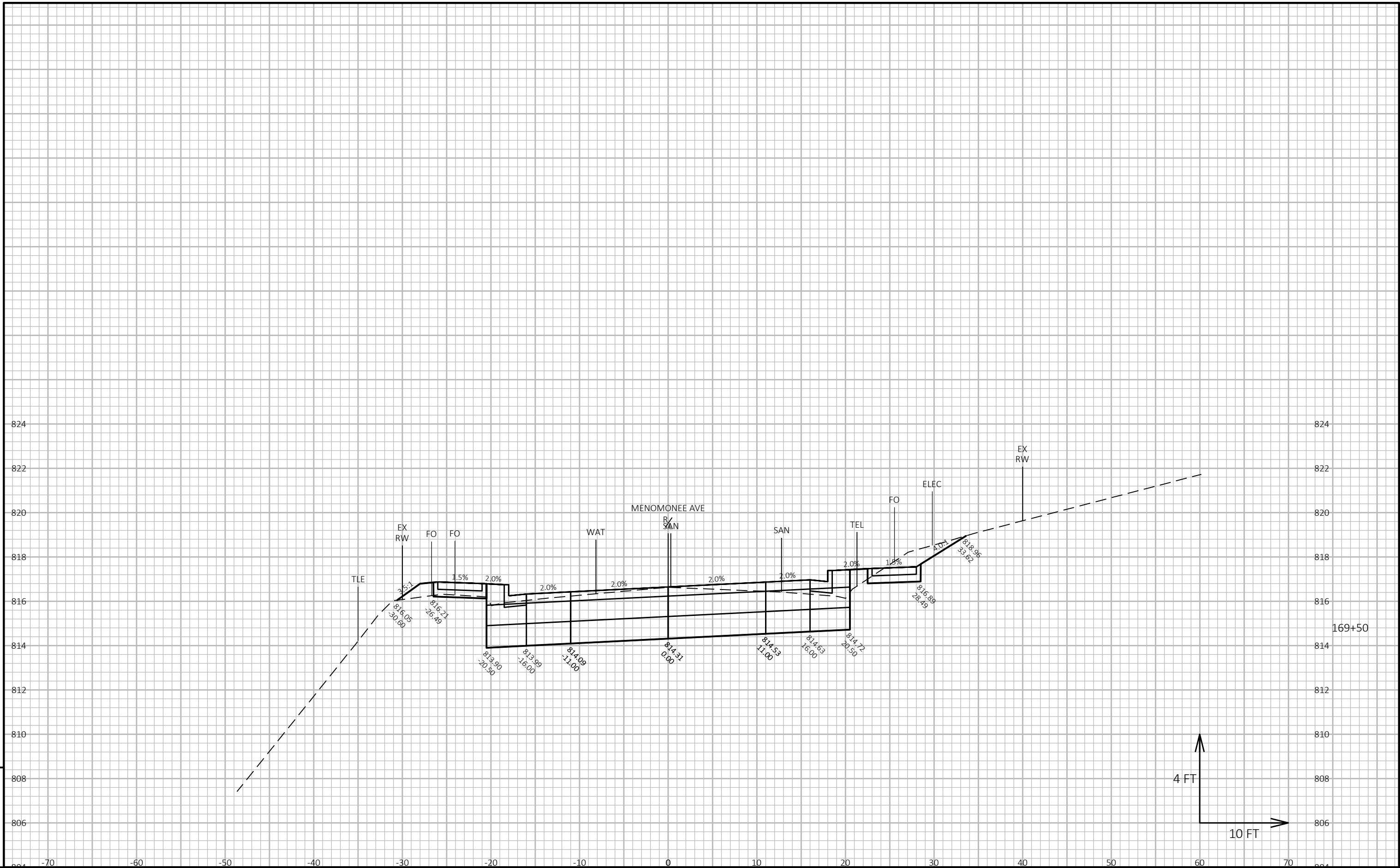
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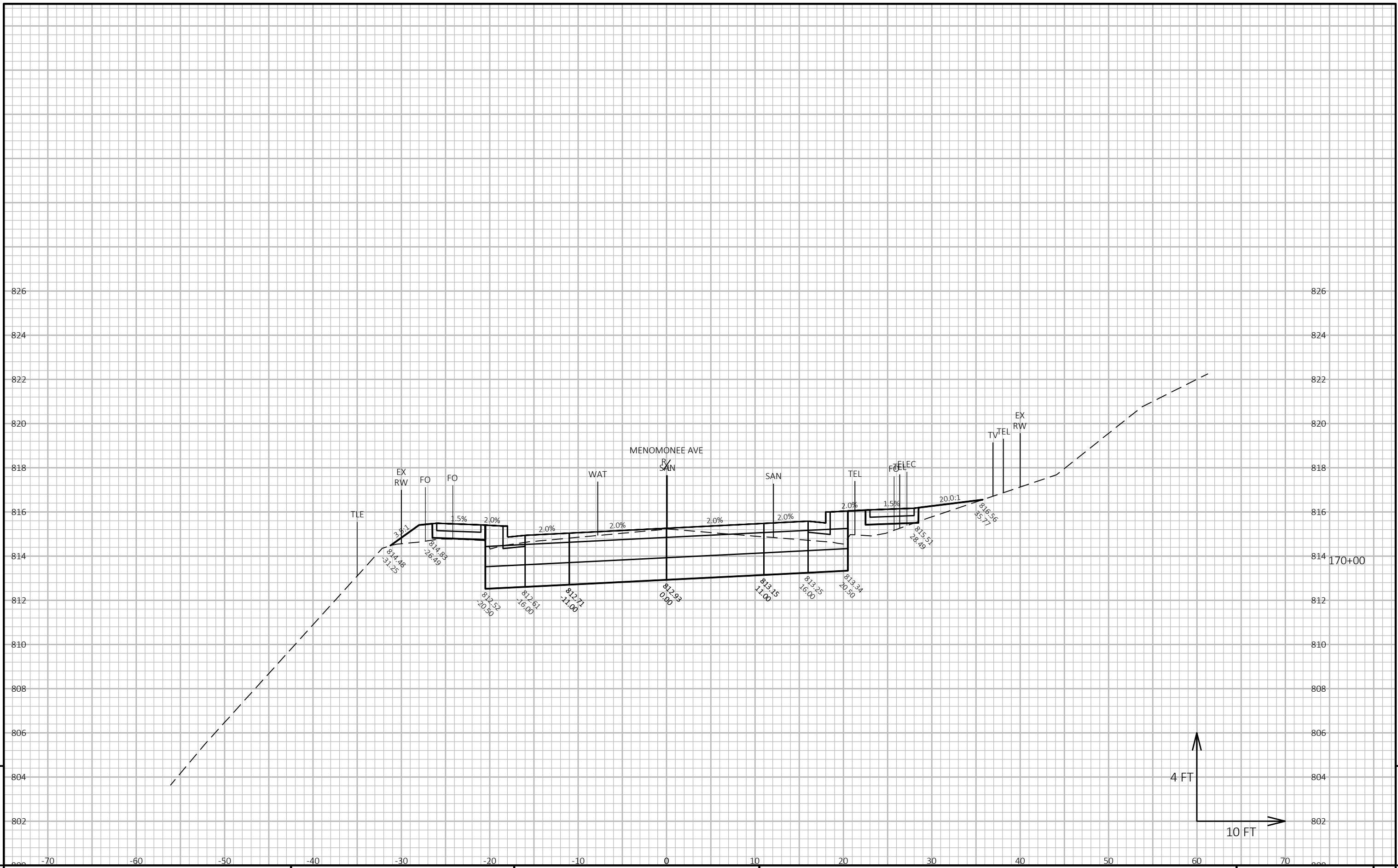
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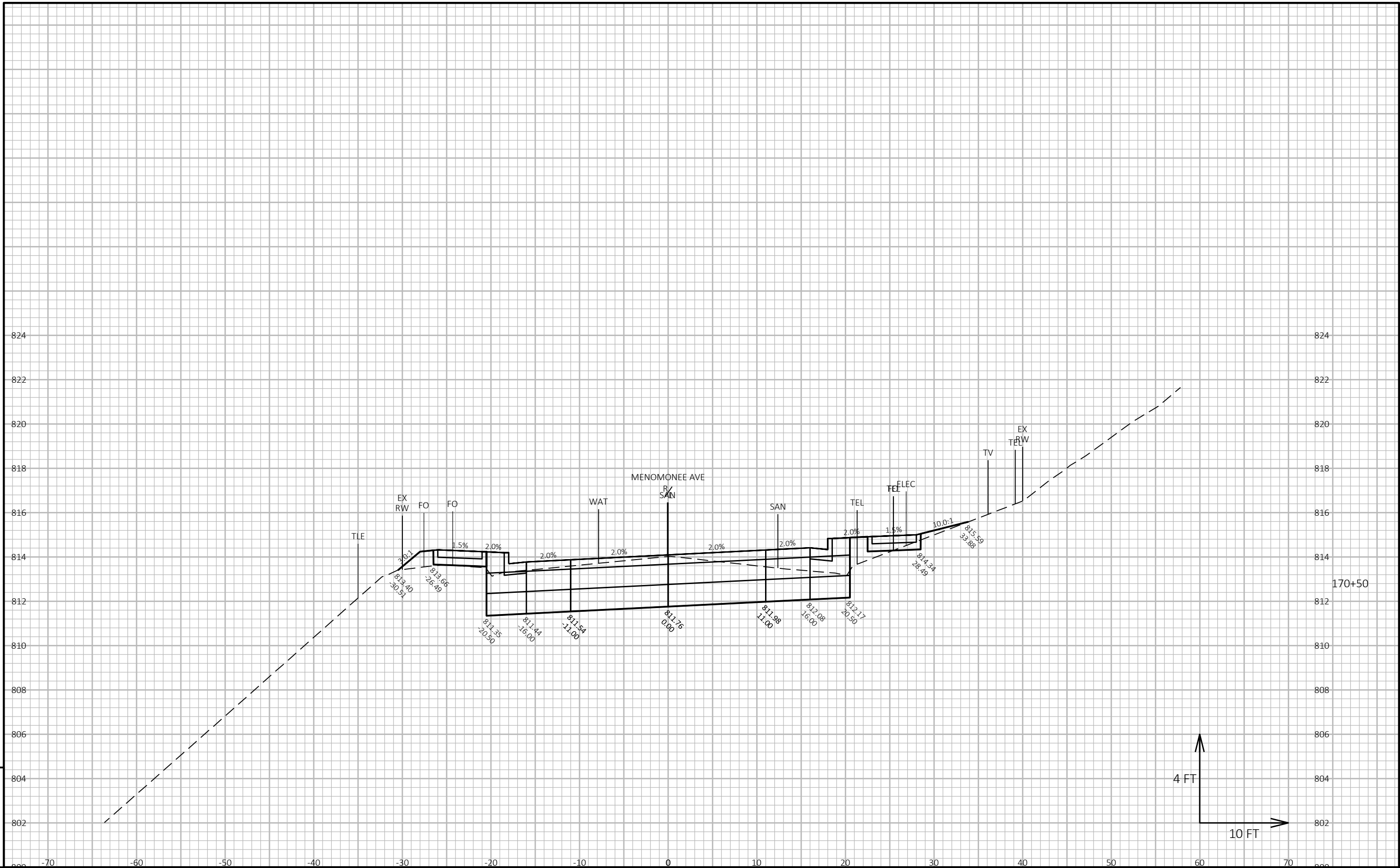
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LAYOUT NAME - 81



PROJECT NO: 2720-07-71	HWY: MENOMONEE AVE	COUNTY: WAUKESHA	CROSS SECTIONS: MENOMONEE AVE	SHEET E
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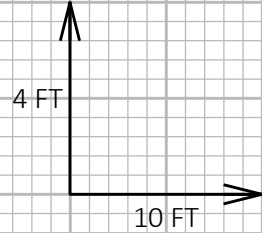


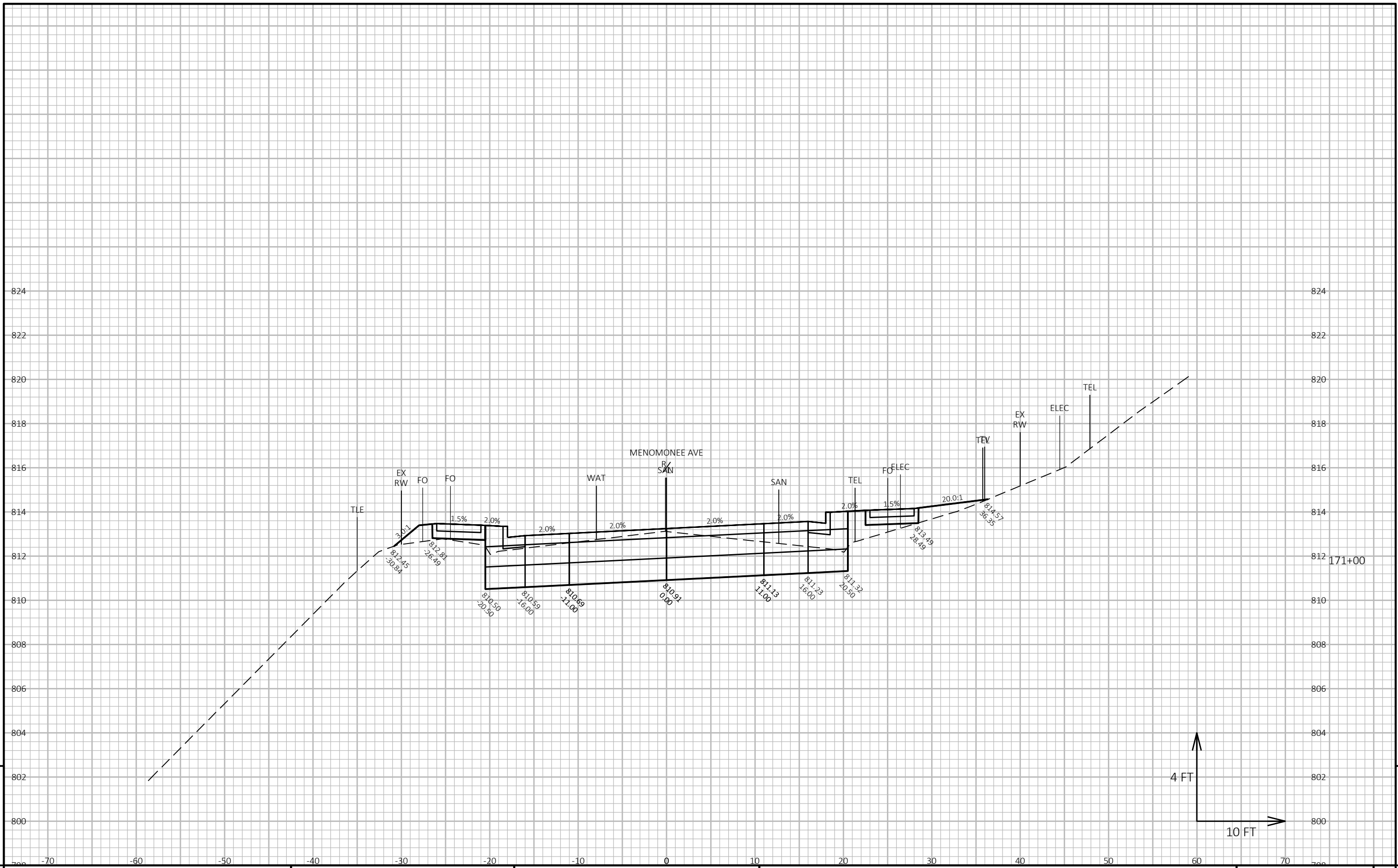
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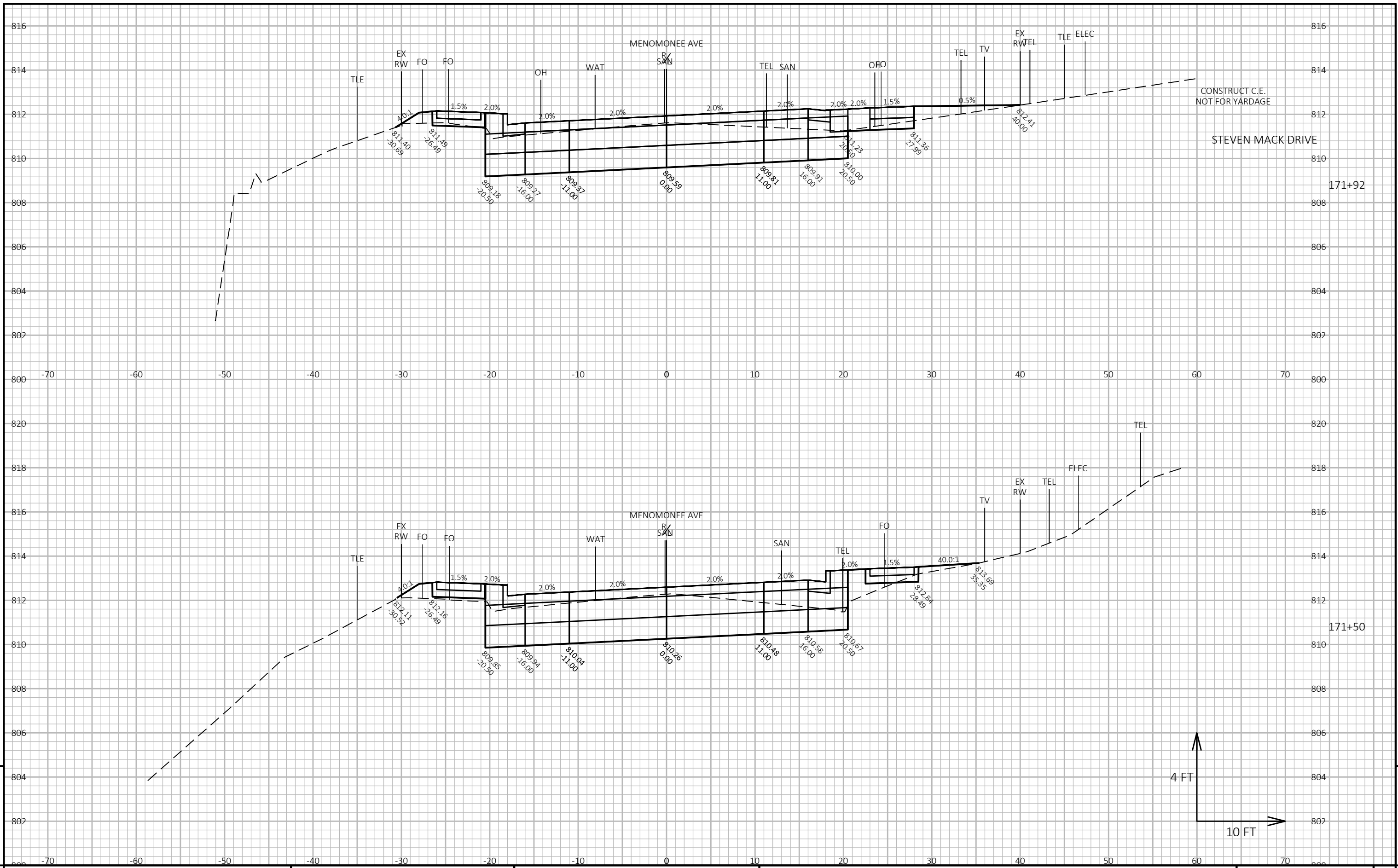
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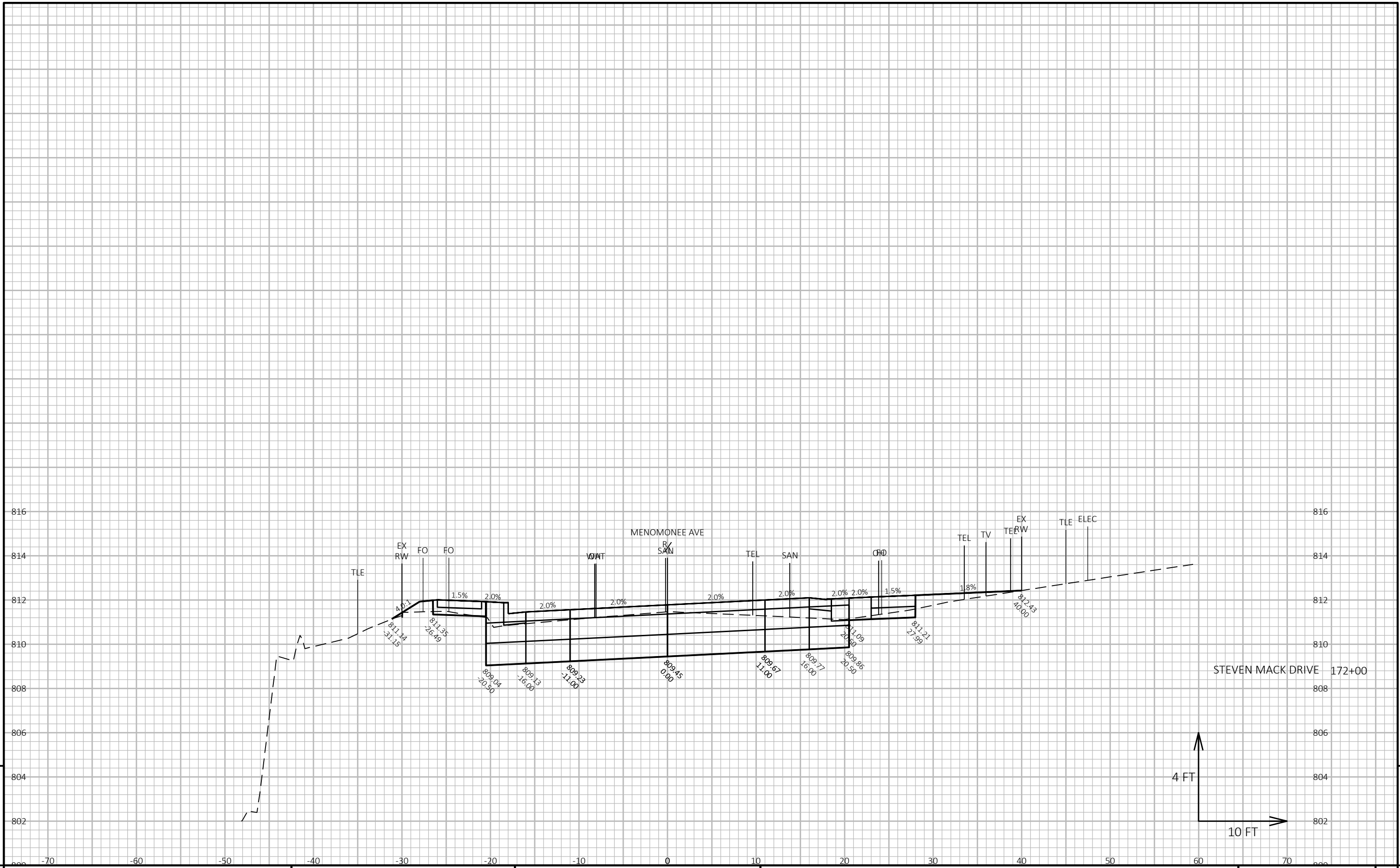
PROJECT NO: 2720-07-71 HWY: MENOMONEE AVE COUNTY: WAUKESHA CROSS SECTIONS: MENOMONEE AVE SHEET E

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LAYOUT NAME - 85



PROJECT NO: 2720-07-71 HWY: MENOMONEE AVE COUNTY: WAUKESHA CROSS SECTIONS: MENOMONEE AVE SHEET E



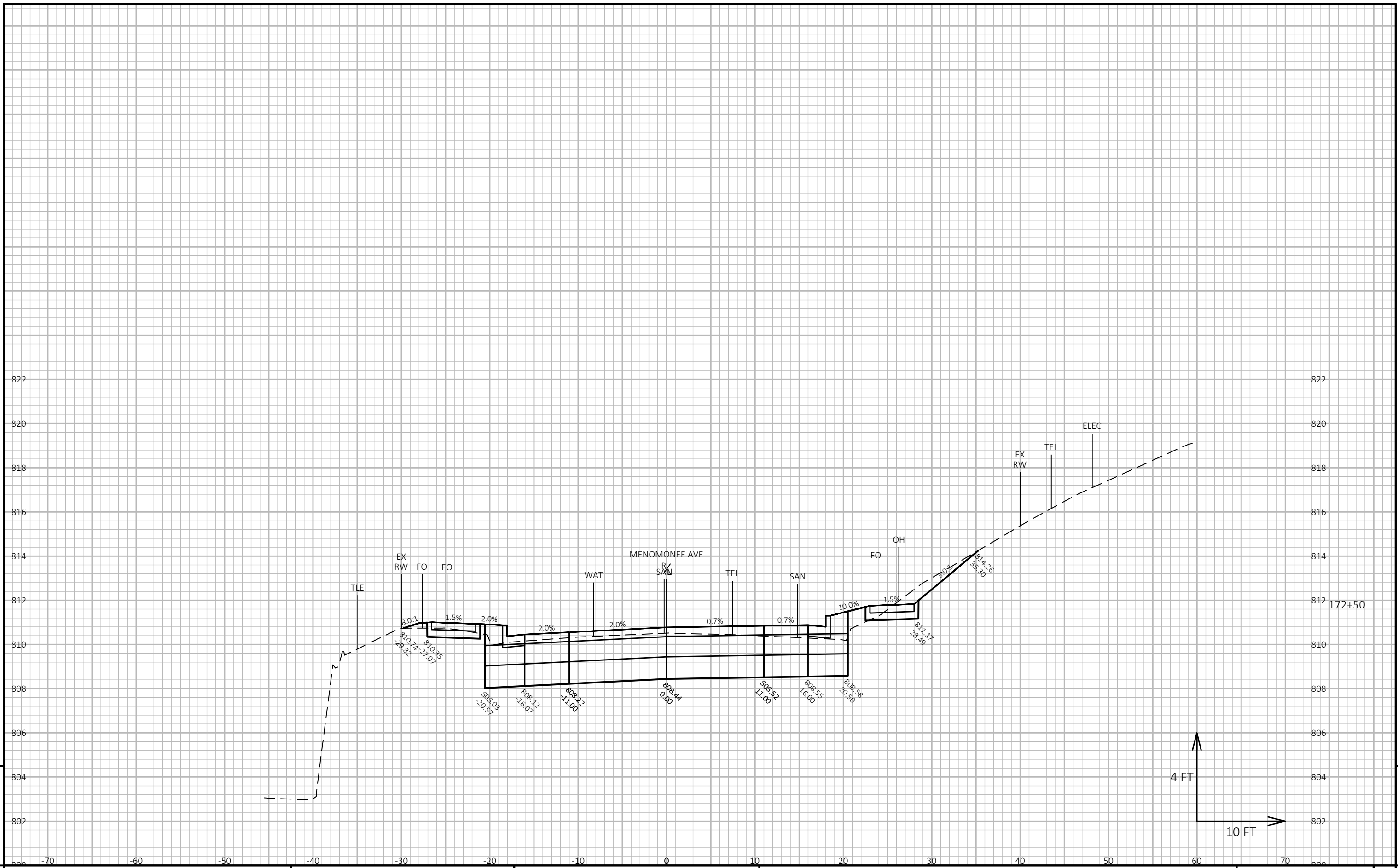
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PROJECT NO: 2720-07-71 HWY: MENOMONEE AVE COUNTY: WAUKESHA CROSS SECTIONS: MENOMONEE AVE SHEET E

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LAYOUT NAME - 87



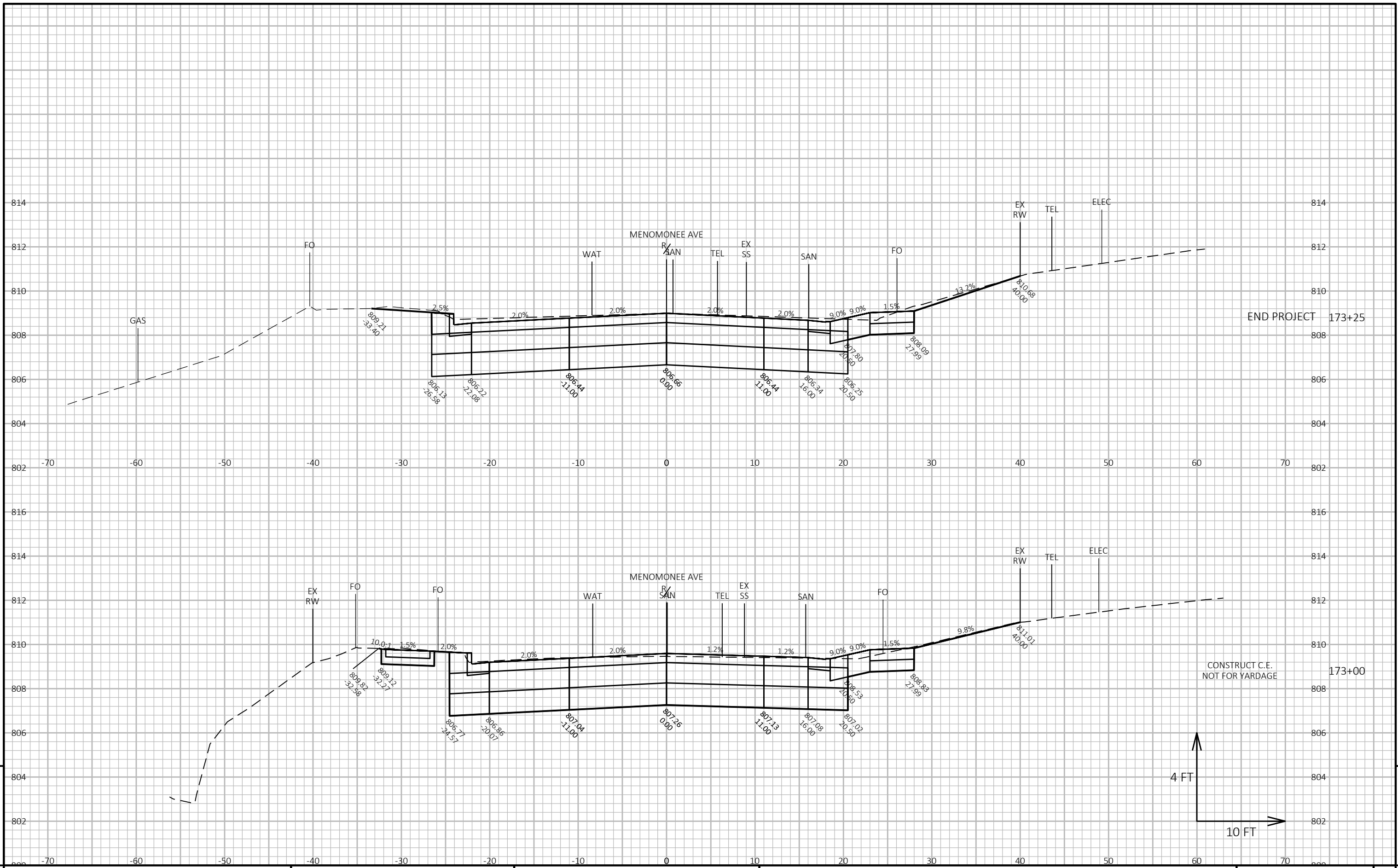
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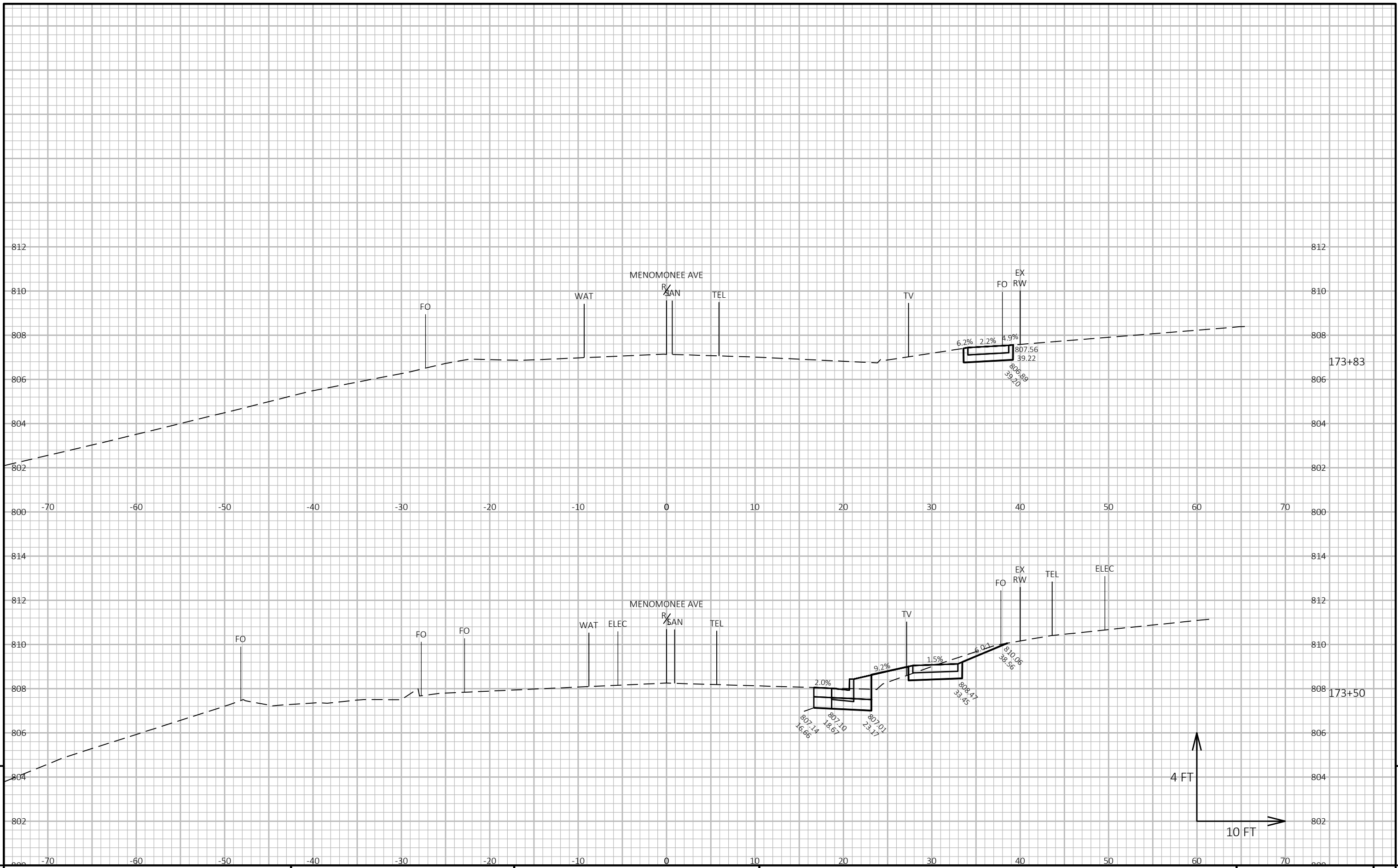
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LAYOUT NAME - 88



PROJECT NO: 2720-07-71 HWY: MENOMONEE AVE COUNTY: WAUKESHA CROSS SECTIONS: MENOMONEE AVE SHEET E



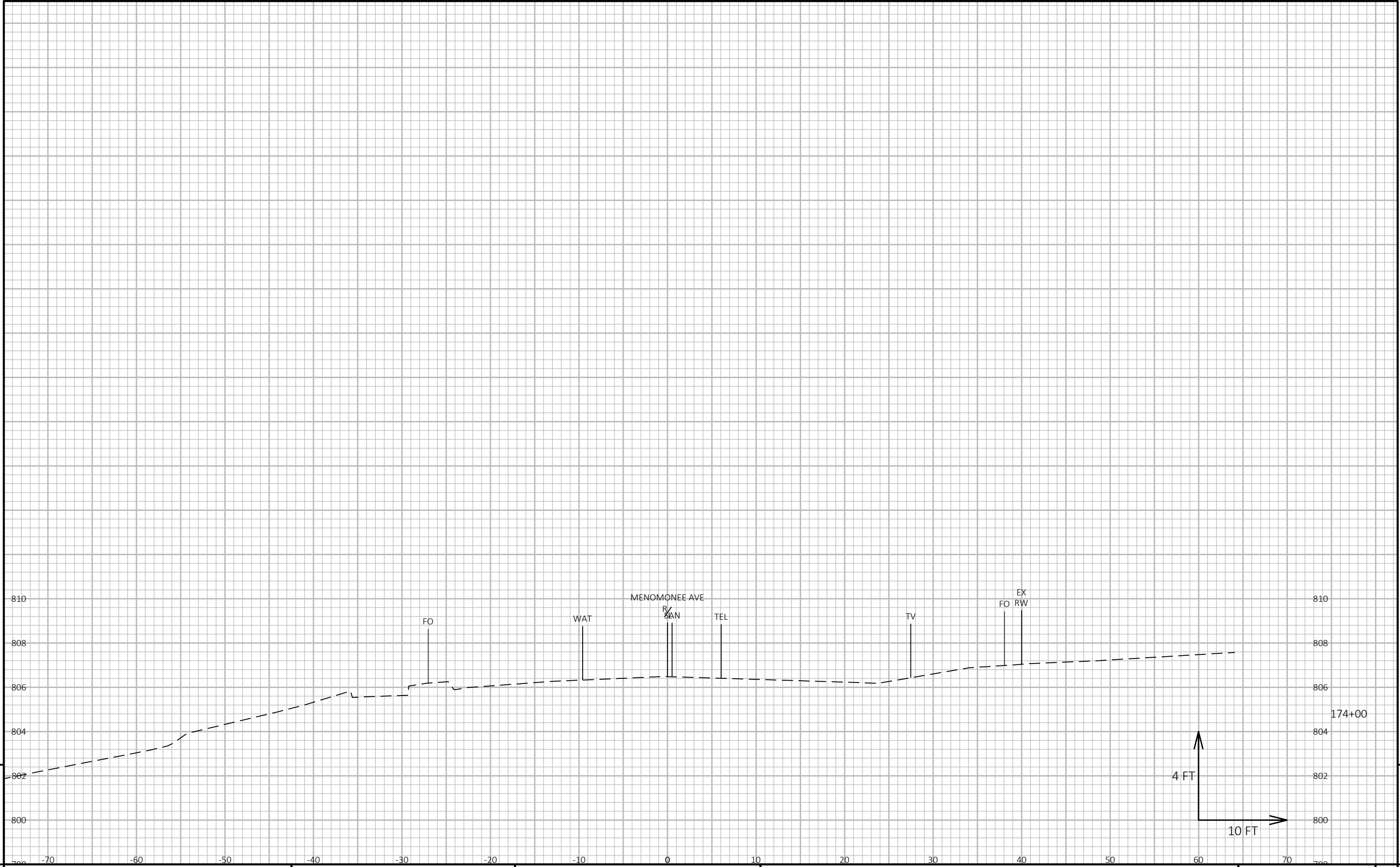
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PROJECT NO: 2720-07-71 HWY: MENOMONEE AVE COUNTY: WAUKESHA CROSS SECTIONS: MENOMONEE AVE SHEET E

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LAYOUT NAME - 90



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PROJECT NO: 2720-07-71	HWY: MENOMONEE AVE	COUNTY: WAUKESHA	CROSS SECTIONS: MENOMONEE AVE	SHEET E
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Wisconsin Department of Transportation

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