

LAX
PROJECT ID: 5780-03-63
WITH: N/A
COUNTY: CRAWFORD

AUGUST 2025
ORDER OF SHEETS

Section No.	1	Title
Section No.	2	Typical Sections and Details
Section No.	3	Estimate of Quantities
Section No.	3	Miscellaneous Quantities
Section No.	4	Right of Way Plat
Section No.	5	Plan and Profile (Includes Erosion Control Plan)
Section No.	6	Standard Detail Drawings
Section No.	7	Sign Plates
Section No.	8	Structure Plans
Section No.	9	Computer Earthwork Data
Section No.	9	Cross Sections

TOTAL SHEETS = 106



DESIGN DESIGNATION

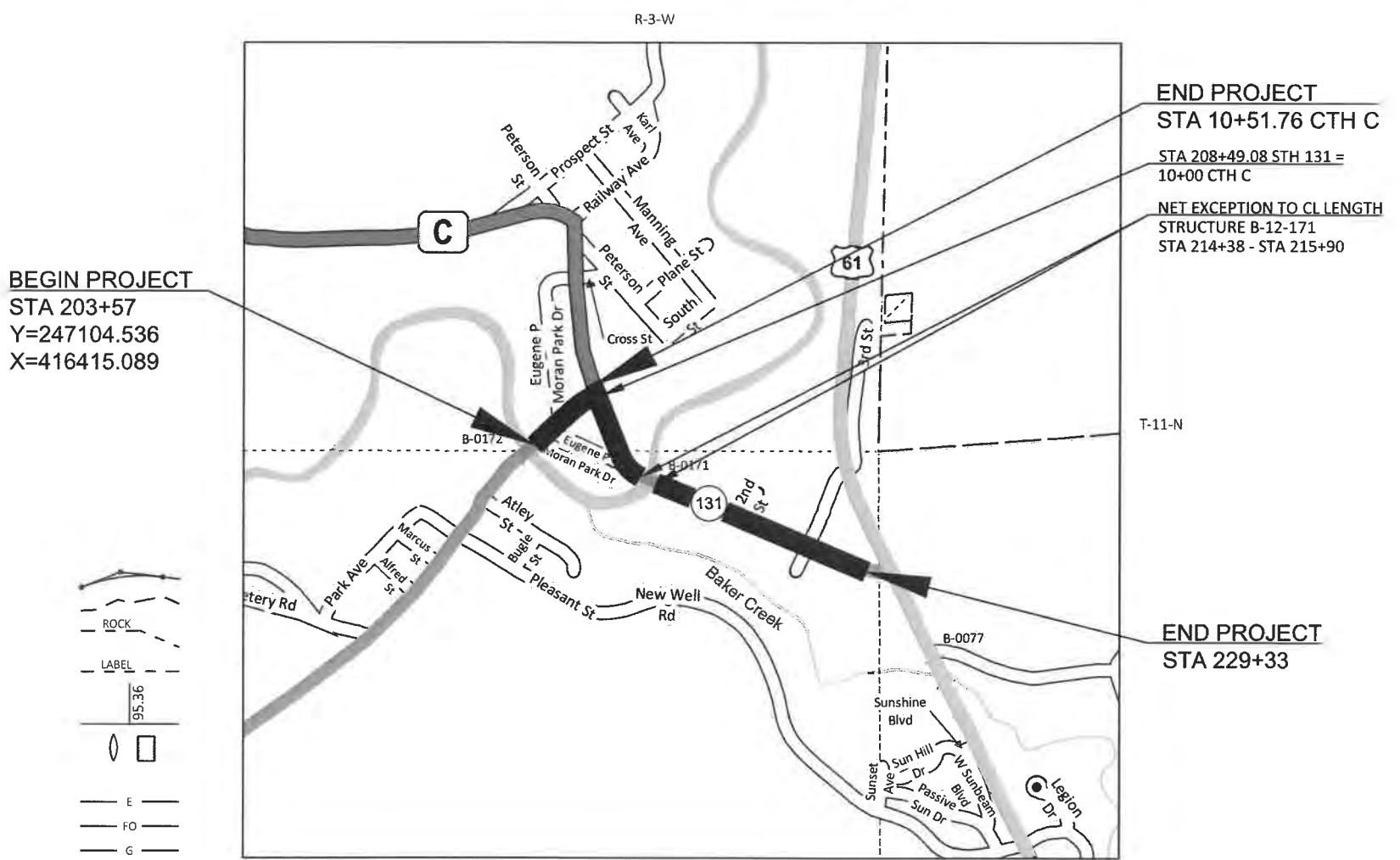
A.A.D.T.	2028	=	1420
A.A.D.T.	2048	=	1420
D.H.V.		=	N/A
D.D.		=	N/A
T.		=	15.5%
DESIGN SPEED		=	30
ESALS		=	420,000

CONVENTIONAL SYMBOLS

PLAN	PROFILE
CORPORATE LIMITS	GRADE LINE
PROPERTY LINE	ORIGINAL GROUND
LOT LINE	MARSH OR ROCK PROFILE (To be noted as such)
LIMITED HIGHWAY EASEMENT	SPECIAL DITCH
EXISTING RIGHT OF WAY	GRADE ELEVATION
PROPOSED OR NEW R/W LINE	CULVERT (Profile View)
SLOPE INTERCEPT	UTILITIES
REFERENCE LINE	ELECTRIC
EXISTING CULVERT	FIBER OPTIC
PROPOSED CULVERT (Box or Pipe)	GAS
COMBUSTIBLE FLUIDS	SANITARY SEWER
MARSH AREA	STORM SEWER
WOODED OR SHRUB AREA	TELEPHONE
	WATER
	UTILITY PEDESTAL
	POWER POLE
	TELEPHONE POLE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
PLAN OF PROPOSED IMPROVEMENT
WAUZEKA - SOLDIERS GROVE
KICKAPOO RIVER B-12-172 TO USH 61
STH 131
CRAWFORD COUNTY

STATE PROJECT NUMBER
5780-03-63



LAYOUT
SCALE 0 1,000 FT
TOTAL NET LENGTH OF CENTERLINE = 0.459 MI

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COUNTY COORDINATE SYSTEM (WCCS), CRAWFORD COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES. ELEVATIONS ARE REFERENCED TO NAVD 88 (2012). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 18.

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
5780-03-63	WISC 2025512	1

ORIGINAL PLANS PREPARED BY

ISG

WISCONSIN
CORONA V. WOYCHIK
E-47688
ONALASKA, WISC.
PROFESSIONAL ENGINEER

DATE: 6/12/2025 (Professional Engineer Signature)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY

Surveyor	I & S GROUP, INC.
Designer	I & S GROUP, INC.
Project Manager	JOHN BANTER, PE
Regional Examiner	SW REGION
Regional Supervisor	JOHN BANTER, PE

APPROVED FOR THE DEPARTMENT

DATE: 6/15/2025 (Signature)

E

GENERAL NOTES

- THERE ARE UTILITY FACILITIES WITHIN THE PROJECT AREA THAT ARE NOT SHOWN ON THE PLANS. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH A CALL TO DIGGERS HOTLINE AND/OR A DIRECT CALL TO THE UTILITIES THAT HAVE FACILITIES IN THE AREA. NOT ALL UTILITIES ARE MEMBERS OF DIGGERS HOTLINE.
- APPLY TACK COAT AT A RATE OF 0.07 GAL/SY TO MILLED PAVEMENT SURFACES.
- HMA PAVEMENT WEIGHT CALCULATIONS ARE BASED ON 112 LB/SY/IN.
- THE CONTRACTOR'S PAVING OPERATIONS SHALL BE CONSISTENT WITH THE PLAN TYPICAL SECTIONS AND CONSTRUCTED TO PREVENT HMA LONGITUDINAL JOINTS FROM BEING LOCATED WITHIN A DRIVING, TURNING, OR PARKING LANE.
- 2-INCH HMA PAVEMENT TYPE 4 LT 58-28 H.
- DISTURBED AREAS WITHIN THE RIGHT-OF-WAY ARE TO BE TOPSOILED (SALVAGED, 6 INCHES MINIMUM) SEEDED, FERTILIZED, AND EROSION MATTED AS DIRECTED BY THE ENGINEER. ALL OTHER DISTURBED AREAS ARE TO BE TOPSOILED (SALVAGED, 6 INCHES MINIMUM) SEEDED, FERTILIZED, AND EROSION MATTED AT THE CONTRACTORS EXPENSE.
- NO TREES, SHRUBS, OR PRIVATE PLANTINGS ARE TO BE REMOVED WITHOUT PRIOR APPROVAL OF THE ENGINEER.
- MISCELLANEOUS REMOVAL ITEMS REQUIRING RESTORATION OF CONCRETE OR ASPHALT DRIVEWAYS OR SIDE STREETS SHALL BE REMOVED TO AN EXISTING JOINT OR TO LIMITS DETERMINED BY THE ENGINEER.

ORDER OF SECTION 2 SHEETS

GENERAL NOTES
PROJECT OVERVIEW
TYPICAL SECTIONS
CONSTRUCTION DETAILS
CURB RAMP DETAILS
PAVEMENT MARKING DETAILS
ADVANCE WARNING SIGNING
PEDESTRIAN DETOUR PLAN

STANDARD ABBREVIATIONS

AC	ACRE	EX	EXISTING	SHLDR	SHOULDER
AGG	AGGREGATE	FERT	FERTILIZER	SB	SOUTHBOUND
AH	AHEAD	FE	FIELD ENTRANCE	SF or SQ FT	SQUARE FEET
AADT	ANNUAL AVERAGE DAILY TRAFFIC	FL or F/L	FLOW LINE	SY or SQ YD	SQUARE YARD
ASPH	ASPHALTIC	FT	FOOT	SDD	STANDARD DETAIL DRAWINGS
AVG	AVERAGE	HMA	HOT MIX ASPHALT	STH	STATE TRUNK HIGHWAYS
BK	BACK	CWT	HUNDREDWEIGHT	SE	SUPERELEVATION
BAD	BASE AGGREGATE DENSE	INL	INLET	T	TANGENT
BM	BENCH MARK	INV	INVERT	TFMP	TEMPORARY
BR	BRIDGE	JCT	JUNCTION	USH	UNITED STATES HIGHWAY
CL or C/L	CENTER LINE	Lf	LEFT	V	VELOCITY OR DESIGN SPEED
CE	COMMERCIAL ENTRANCE	L	LENGTH OF CURVE	VC	VERTICAL CURVE
CONC	CONCRETE	LIN FT or LF	LINEAR FOOT	WB	WESTBOUND
CO	COUNTY	LS	LUMP SUM	YD	YARD
CTH	COUNTY TRUNK HIGHWAY	NC	NORMAL CROWN		
CR	CREEK	N	NORTH		
CABC	CRUSHED AGGREGATE BASE COURSE	NB	NORTHBOUND		
CY or CU YD	CUBIC YARD	NO	NUMBER		
CULV	CULVERT	PT	POINT		
CP	CULVERT PIPE	PC	POINT OF CURVATURE		
C & G	CURB AND GUTTER	PI	POINT OF INTERSECTION		
D	DEGREE OF CURVE	PT	POINT OF TANGENCY		
DIA	DIAMETER	PCC	PORTLAND CEMENT CONCRETE		
DISCH	DISCHARGE	LB	POUND		
E	EAST	PC	PRIVATE ENTRANCE		
EB	EASTBOUND	R	RADIUS		
EL or ELEV	ELEVATION	RL or R/L	REFERENCE LINE		
EW	ENDWALL	RT	RIGHT		
ENT	ENTRANCE	R/W	RIGHT-OF-WAY		
EXC	EXCAVATION	RD	ROAD		

UTILITY CONTACTS

ELECTRICITY

ALLIANT ENERGY
ALLAN MUMM
2200 EAST CAMPION BLVD
PRAIRIE DU CHIEN, WI 53821
PHONE: (608) 326-9481
(608) 732-7925
EMAIL: ALLANMUMM@ALLIANTENERGY.COM

COMMUNICATIONS

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JOSH LIEN
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BLUE RIVER, WI 53518
PHONE: (608) 537-2461
EMAIL: JOSH.L@RGTC.COOP

GAS/PETROLEUM

MADISON GAS AND ELEC
ROGER AHLES
623 RAILROAD ST
MADISON, WI 53703
PHONE: (608) 252-5682
EMAIL: RAHLES@MGE.COM

WATER & SANITARY SEWER

SOLDIERS GROVE SEWER & WATER
JORDAN FURE
DELTA 3 ENGINEERING
875 SOUTH CHESTNUT ST
PLATTEVILLE, WI 53818
PHONE: (608) 345-5355
(608) 732-2166
EMAIL: FURJ@DELTA3ENG.BIZ

COMMUNICATIONS

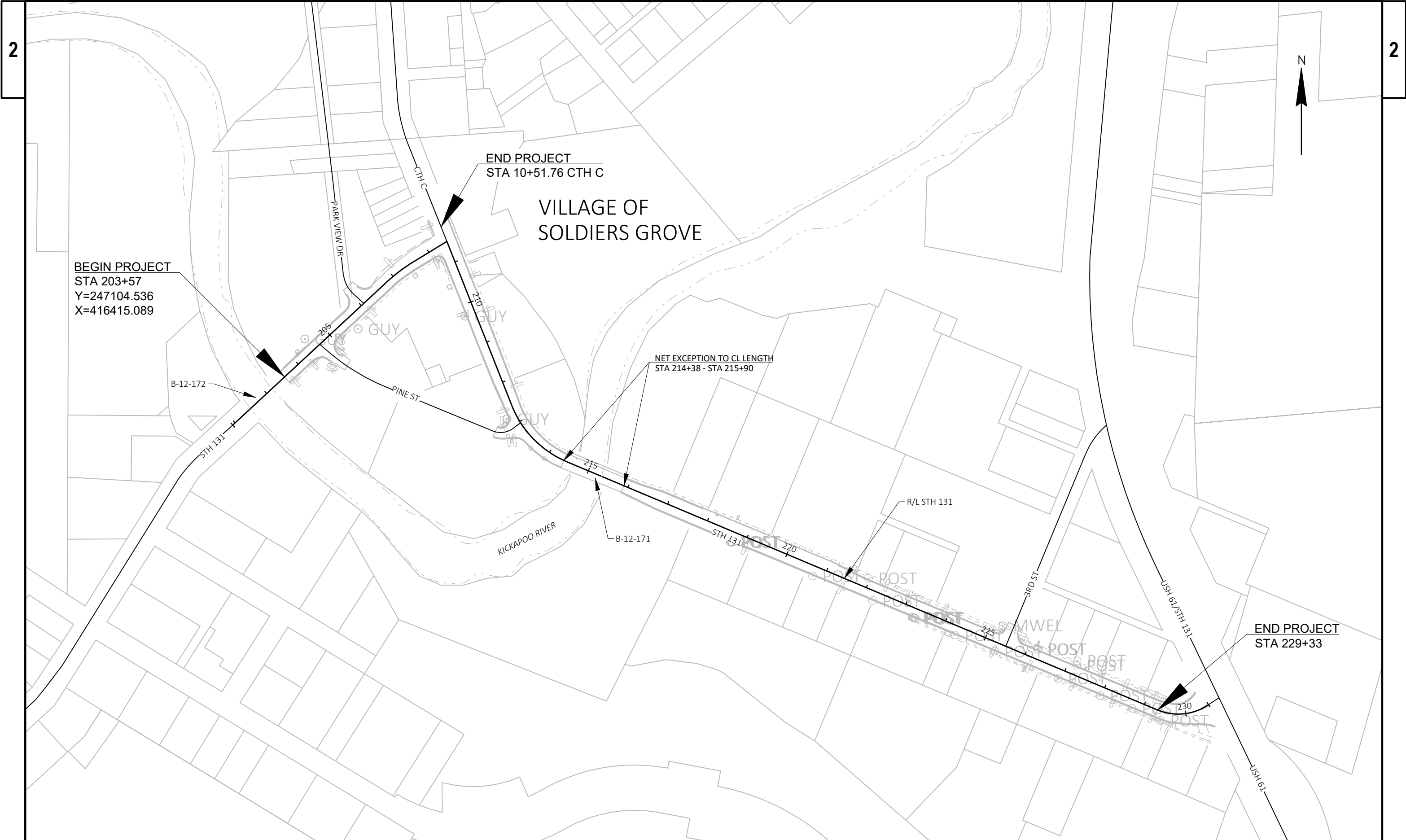
MEDIACOM
CRAIG EGGERT
1240 HIGHWAY 52 SOUTH
CHATFIELD, MN 55923
PHONE: (563) 419-5160
EMAIL: CEGGERT@MEDIACOMCC.COM

DESIGN CONTACTS

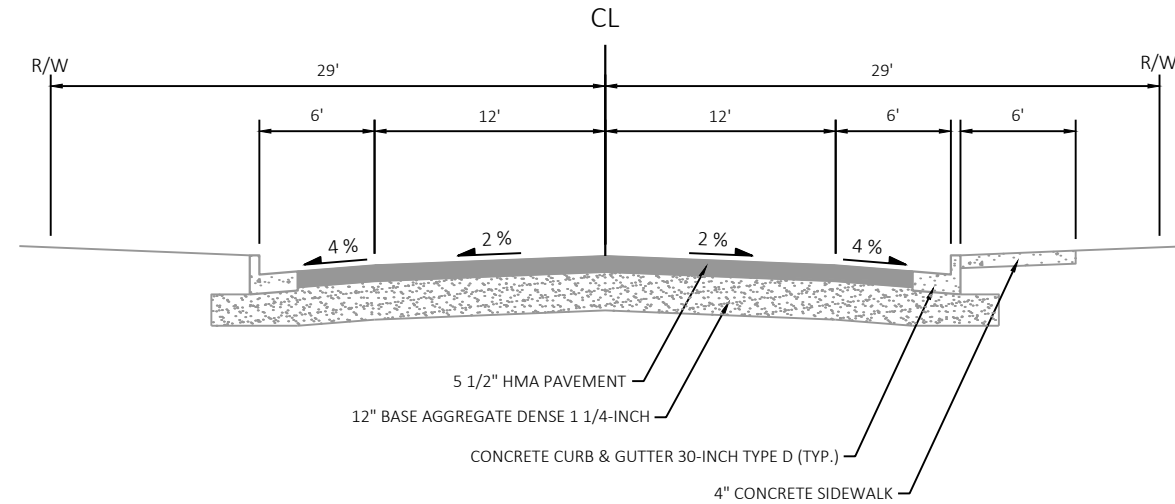
JOHN BANTER, PE WISDOT PROJECT MANAGER 3550 MORMON COULEE RD LA CROSSE, WI 54601 PHONE: (608) 785-9729 EMAIL: JOHN.BANTER@DOT.WI.GOV	CORONA WOYCHIK, PE CIVIL ENGINEER 201 MAIN STREET, SUITE 1020 LA CROSSE, WI 54601 PHONE: (608) 789-2034 EMAIL: CORONA.WOYCHIK@ISGINC.COM
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DNR LIAISON

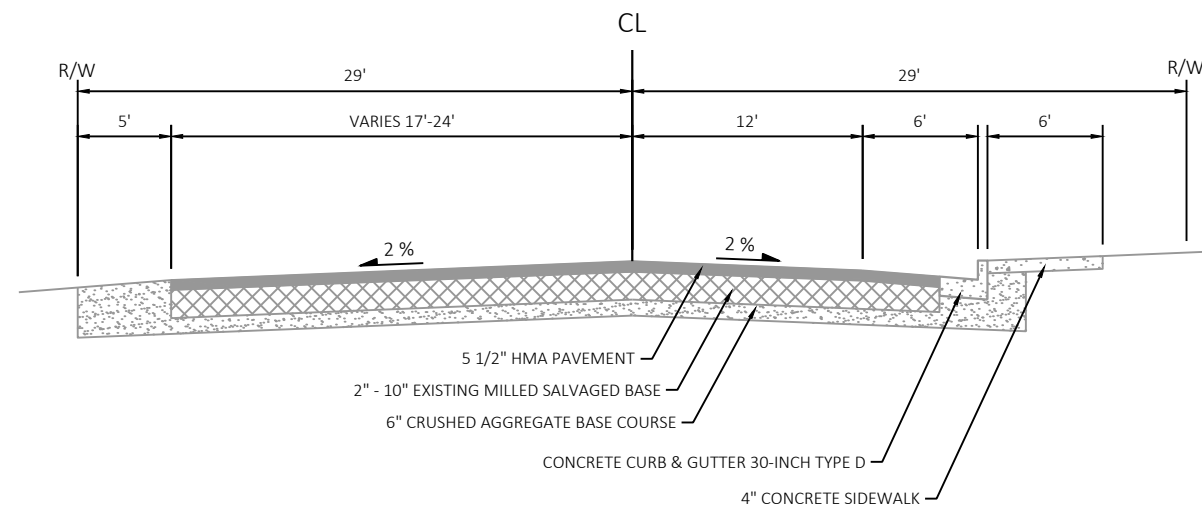
KAREN KALVELAGE
ENVIORNMENTAL ANALYSIS & REVIEW SPECIALIST
WISCONSIN DEPT. OF NATURAL RESOURCES
3550 MORMON COULEE RD
LA CROSSE, WI 54601
PHONE: (608) 785-9115
EMAIL: KAREN.KALVELAGE@WISCONSIN.GOV



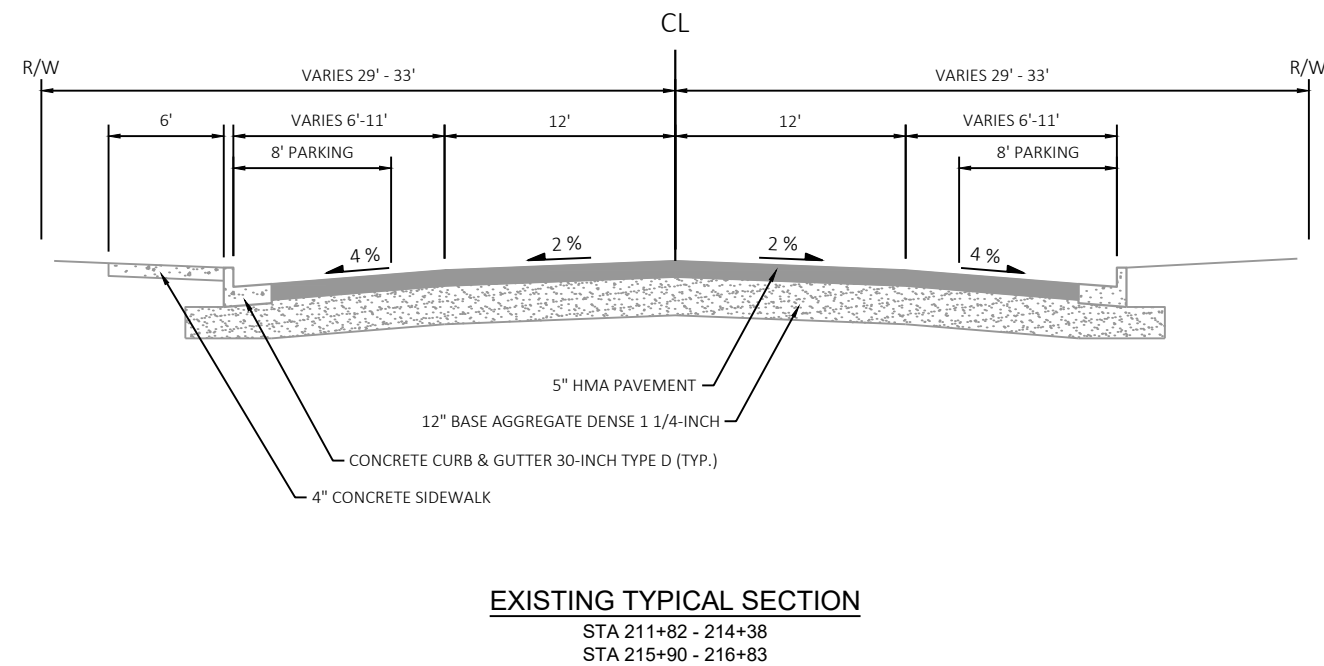
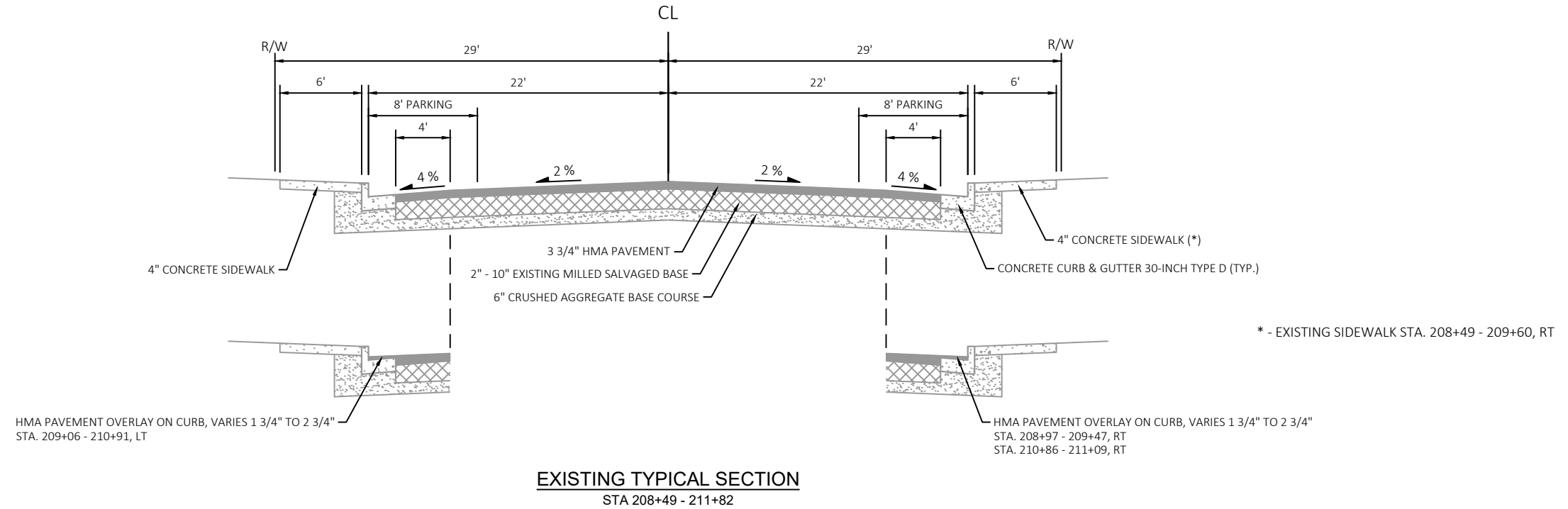
PROJECT NO: 5780-03-63	HWY: STH 131	COUNTY: CRAWFORD	PROJECT OVERVIEW	SHEET	E
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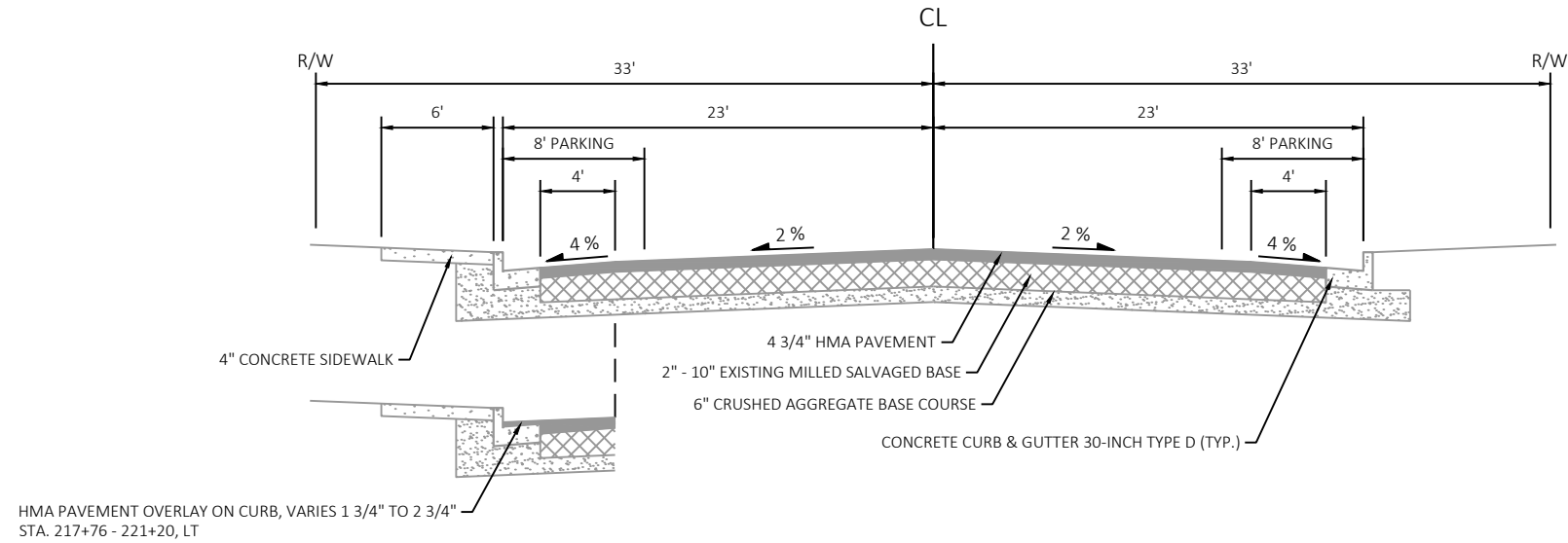
**EXISTING TYPICAL SECTION**

STA 203+57 - 206+49

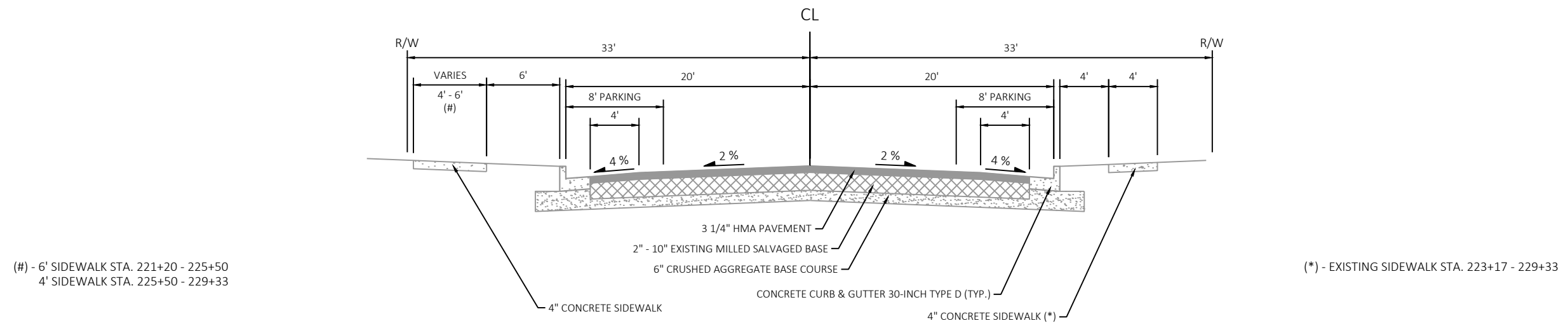
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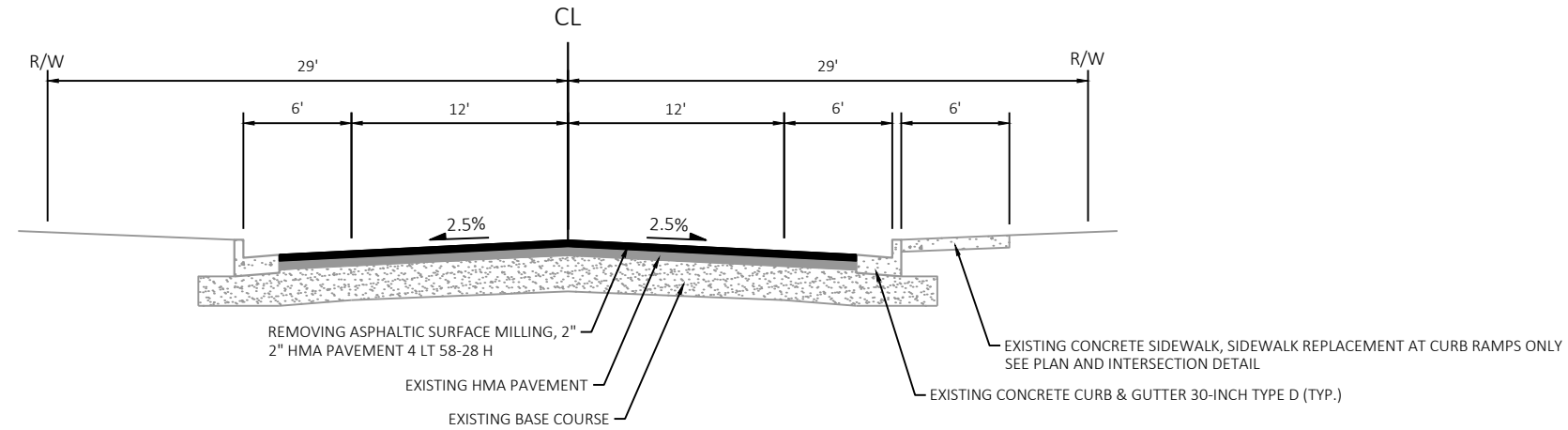




EXISTING TYPICAL SECTION
STA 216+83 - 221+20

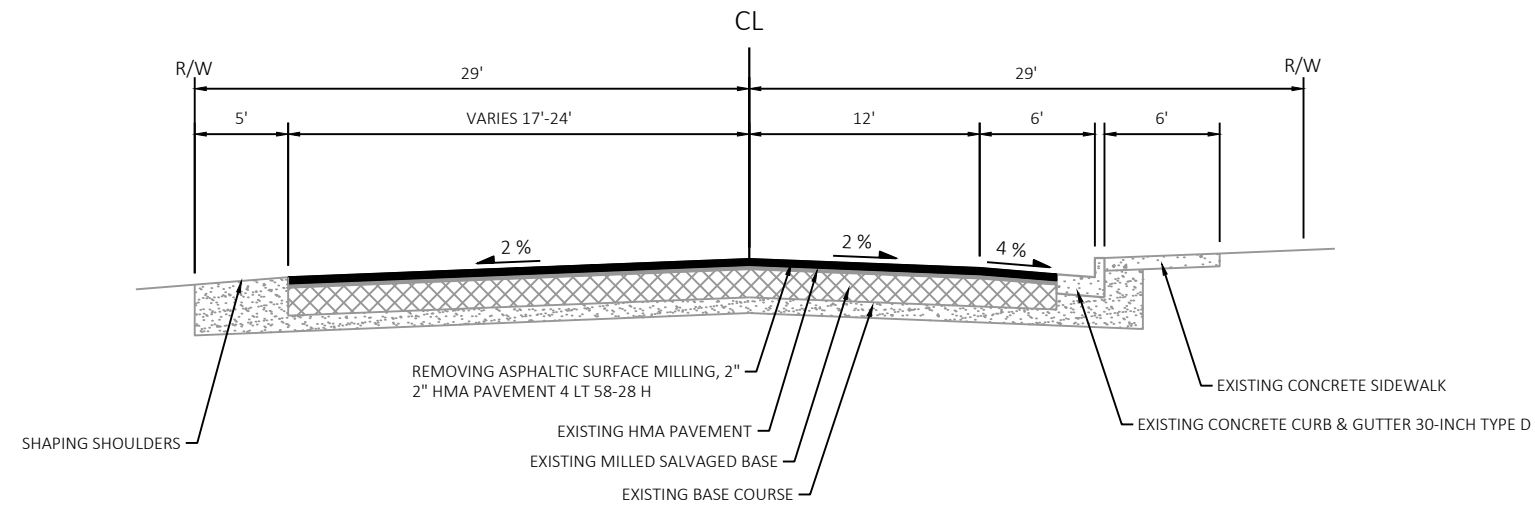


EXISTING TYPICAL SECTION
STA 221+20 - 229+33



FINISHED TYPICAL SECTION

STA 203+57 - 206+49



FINISHED TYPICAL SECTION

STA 206+49 - 208+49

PROJECT NO: 5780-03-63

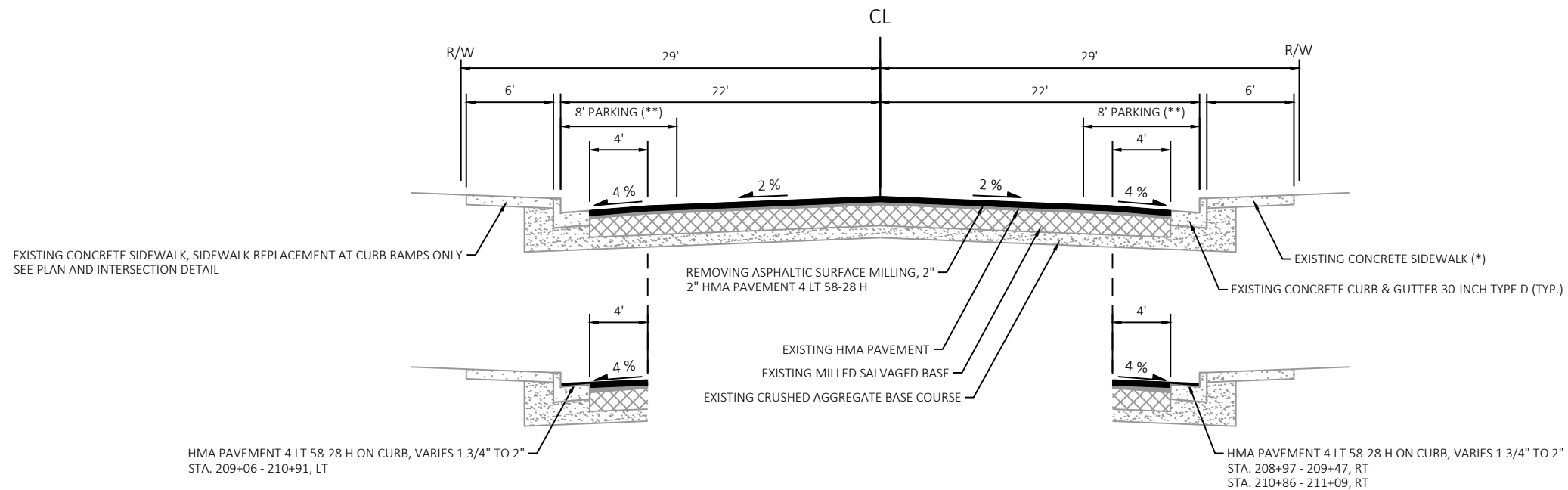
HWY: STH 131

COUNTY: CRAWFORD

TYPICAL SECTIONS

SHEET

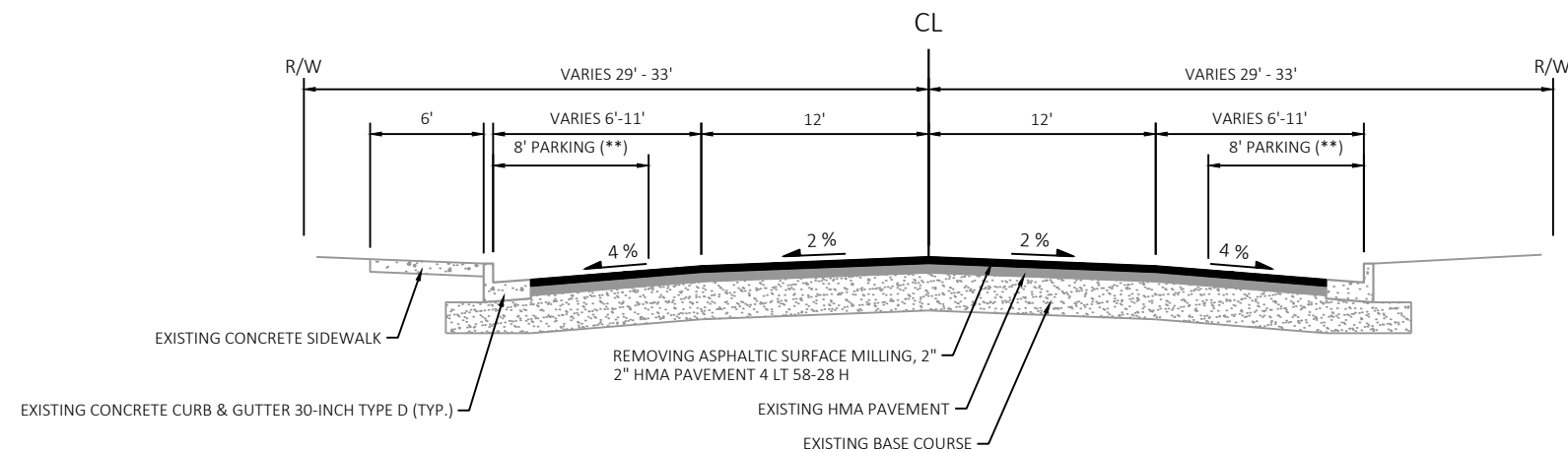
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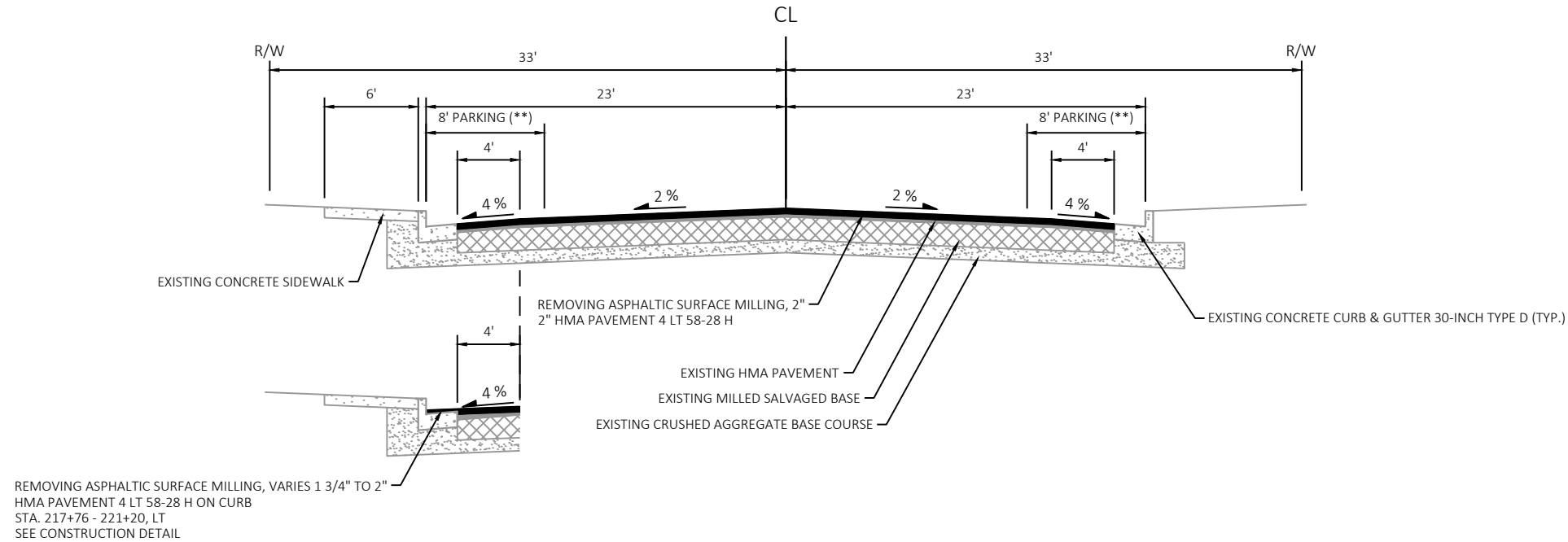
**FINISHED TYPICAL SECTION**

STA 208+49 - 211+82

(*) - EXISTING SIDEWALK STA. 208+49 - 209+60, RT
SIDEWALK REPLACEMENT AT CURB RAMPS ONLY
SEE PLAN AND INTERSECTION DETAIL

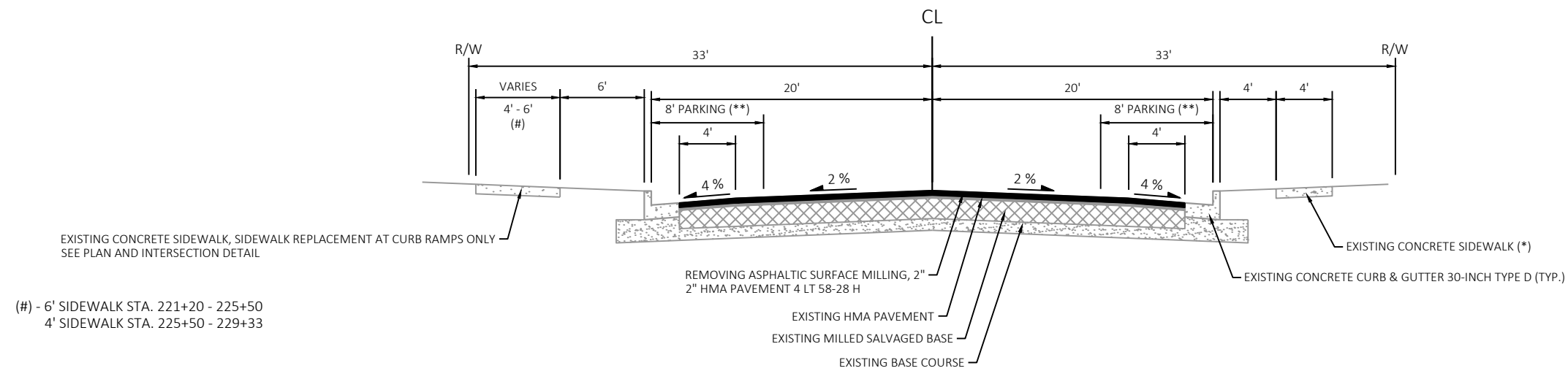
(**) - PARKING AT: STA 209+20 TO 212+18 LT
STA 209+79 TO 212+18 RT
STA 216+79 TO 221+19 LT
STA 221+61 TO 224+98 LT
STA 226+06 TO 229+08 LT
STA 216+79 TO 229+08 RT

**FINISHED TYPICAL SECTION**STA 211+82 - 214+83
STA 215+90 - 216+83

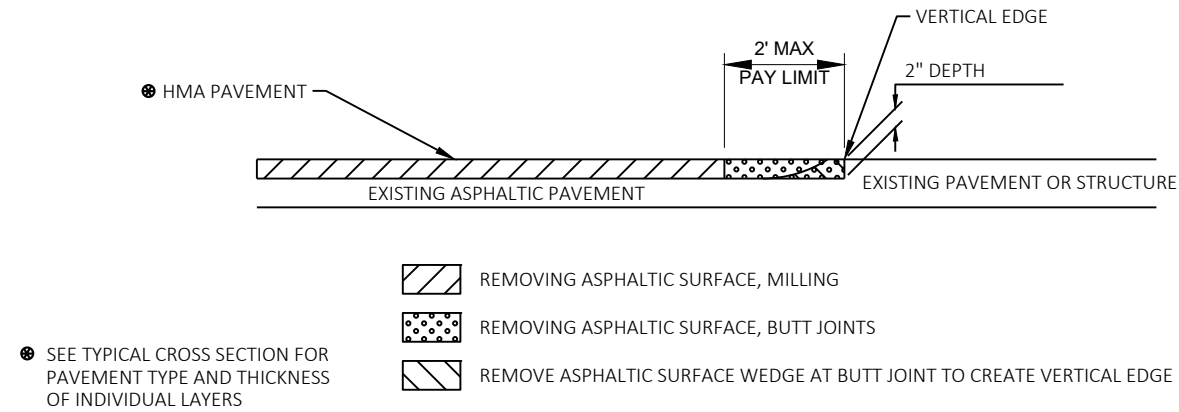
**FINISHED TYPICAL SECTION**

STA 216+83 - 221+20

(*) - EXISTING SIDEWALK STA. 223+17 - 229+33
SIDEWALK REPLACEMENT AT CURB RAMPS ONLY
SEE PLAN AND INTERSECTION DETAIL
(**) - PARKING AT: STA 209+20 TO 212+18 LT
STA 209+79 TO 212+18 RT
STA 216+79 TO 221+19 LT
STA 221+61 TO 224+98 LT
STA 226+06 TO 229+08 LT
STA 216+79 TO 229+08 RT

**FINISHED TYPICAL SECTION**

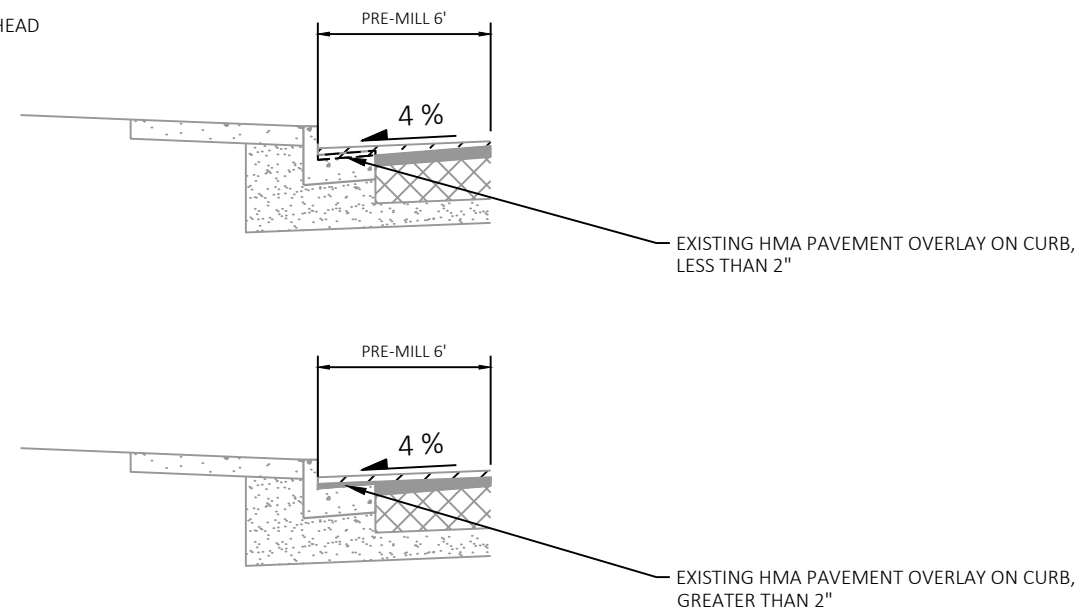
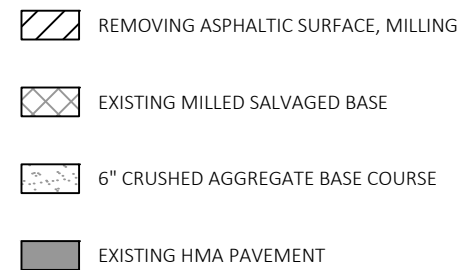
STA 221+20 - 229+33



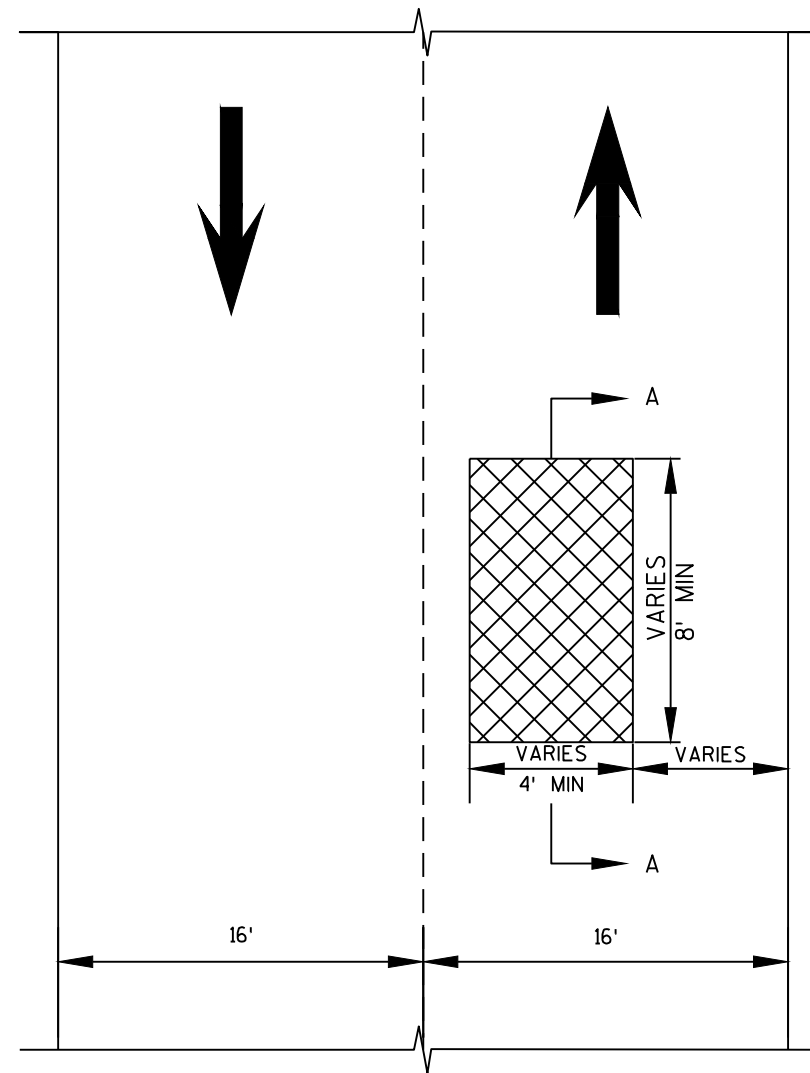
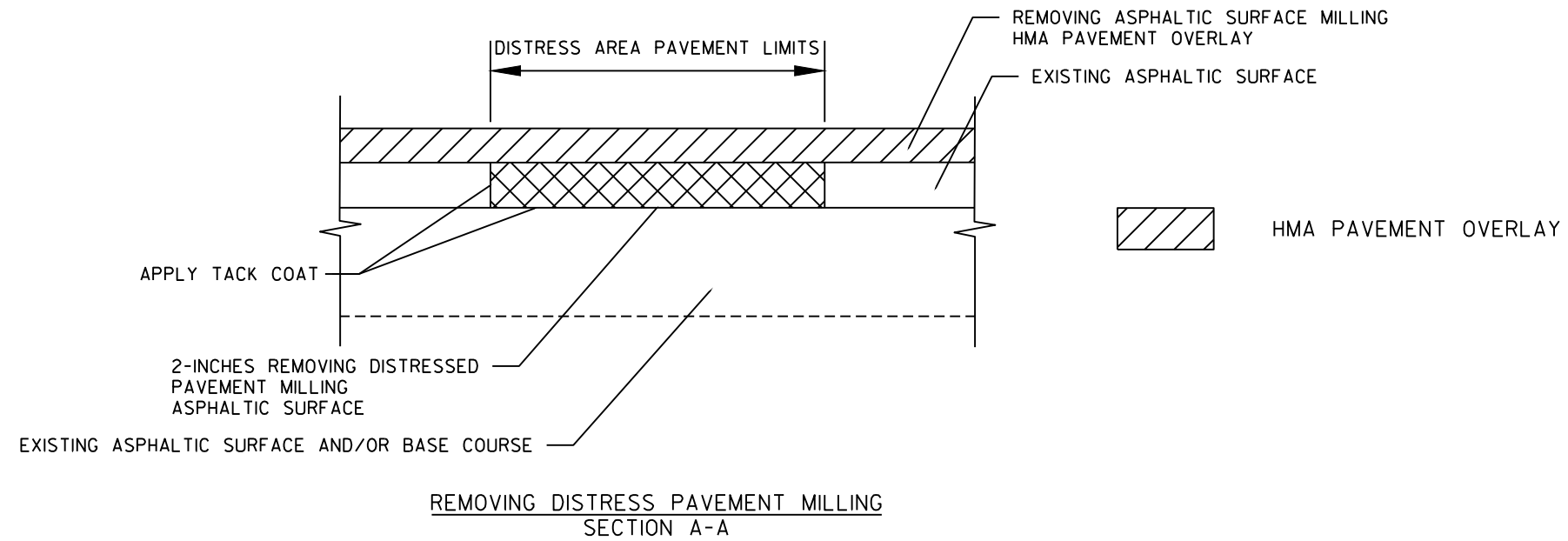
BUTT JOINT DETAIL FOR ASPHALTIC PAVEMENTS

NOTE:

- TAKE CARE WHILE MILLING IN THE CURB TO PROTECT CURB HEAD



CURB AND GUTTER MILLING DETAIL ADJACENT TO ASPHALTIC PAVEMENT



REMOVING DISTRESSED PAVEMENT MILLING

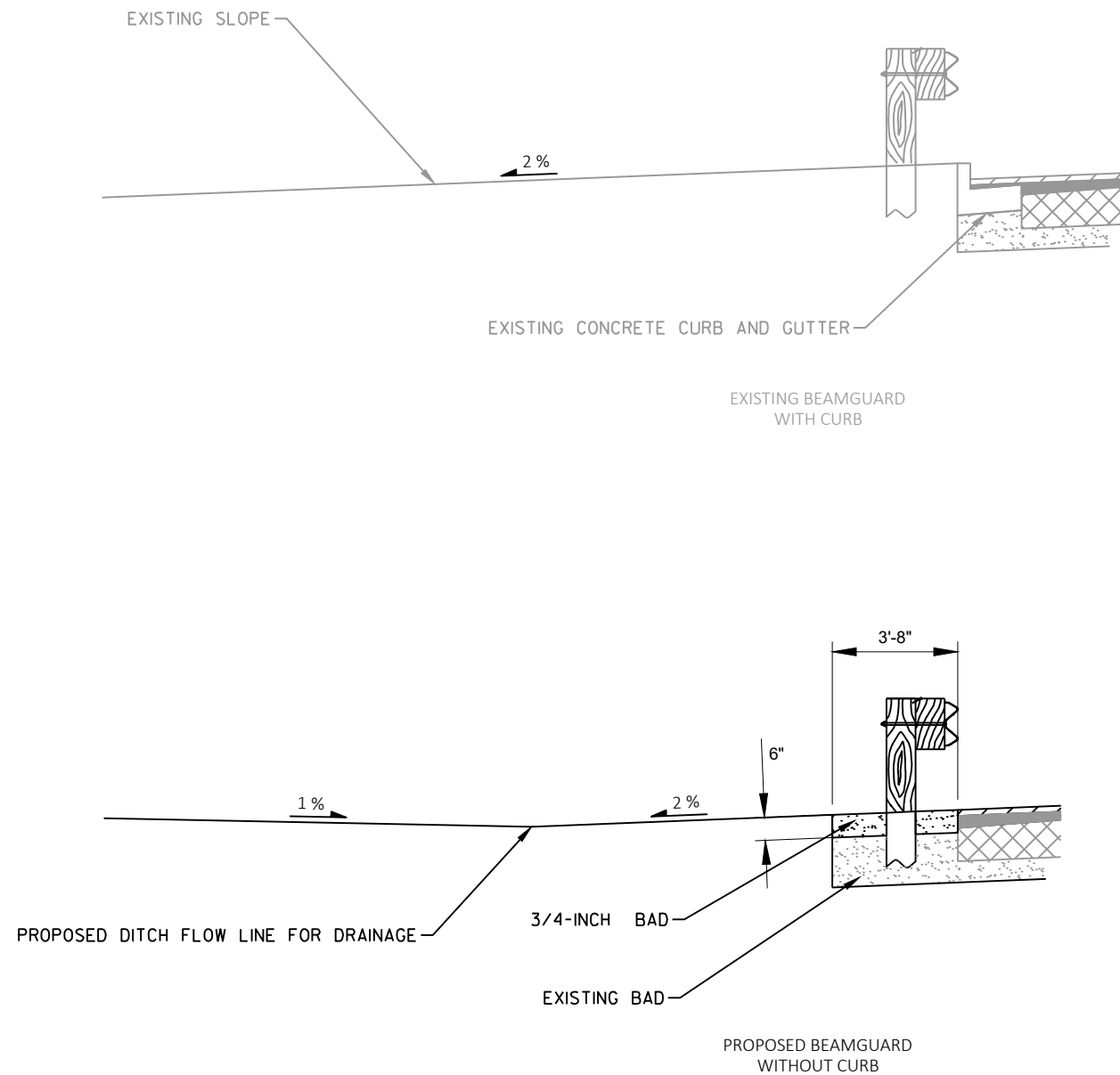


TRAVEL DIRECTION

NOTE: ONLY REMOVE DISTRESS PAVEMENT MILLING WITHIN 16' OF CENTERLINE

PLAN VIEW

EXACT LOCATION AND LIMITS OF REMOVING DISTRESSED PAVEMENT MILLING TO BE DETERMINED BY THE ENGINEER IN THE FIELD

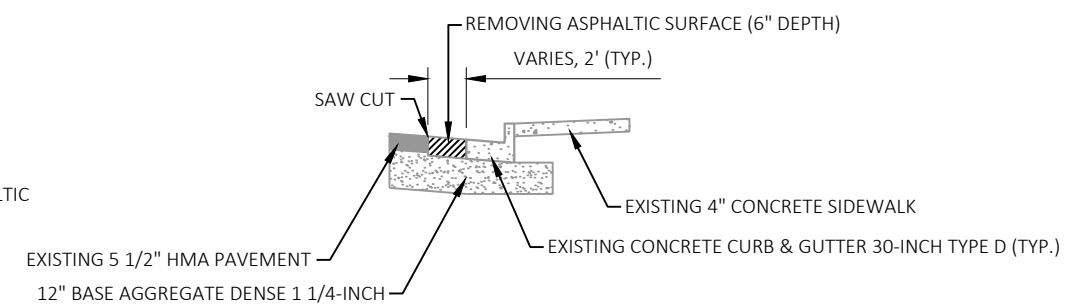


BARRIER SYSTEM GRADING SHAPING FINISHING DETAIL

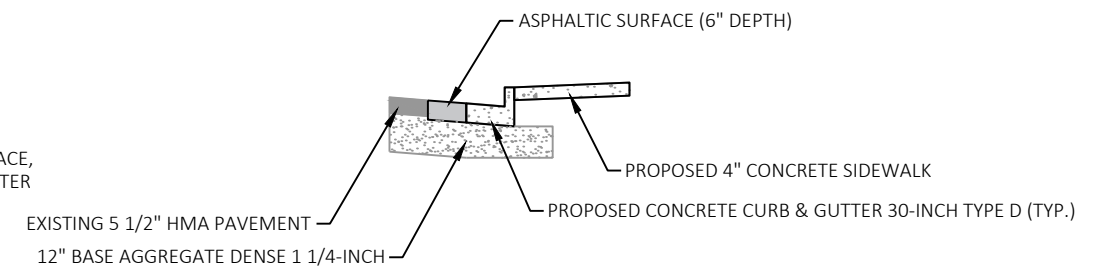
STA 213+09 - 213+80

NOTE: IN AREAS WHERE CONCRETE CURB AND GUTTER IS REMOVED, GRADING, SHAPING AND FINISHING OF THE EXISTING SLOPE MUST BE DONE FOR DRAINAGE PURPOSES.

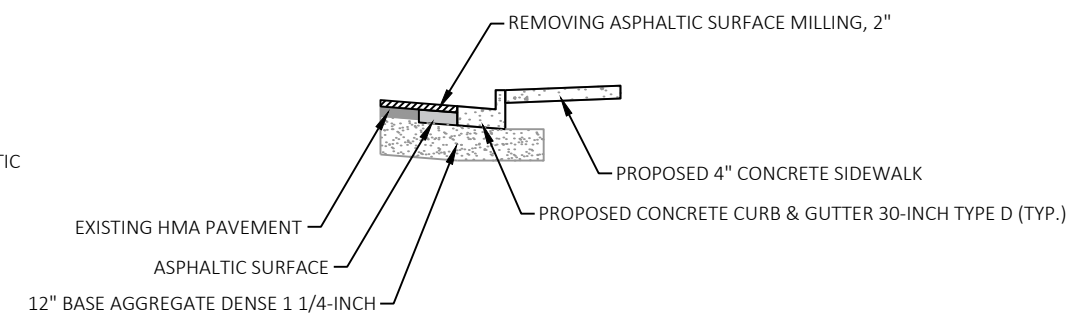
STEP 1: REMOVE ASPHALTIC SURFACE, 6" DEPTH



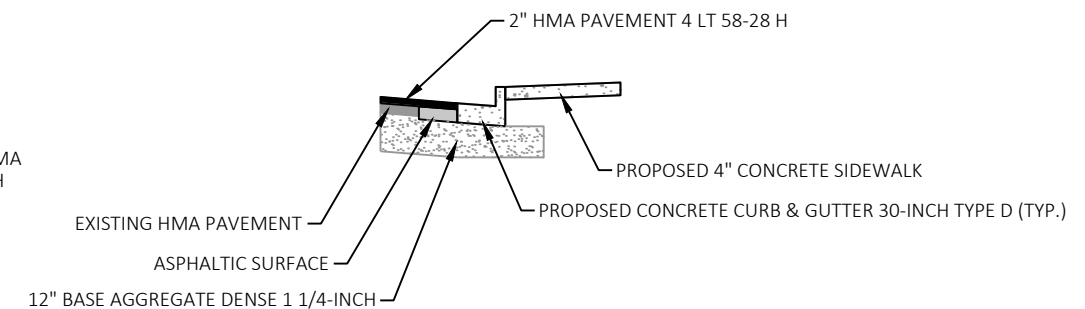
STEP 2: ASPHALTIC SURFACE, REPLACE CURB AND GUTTER



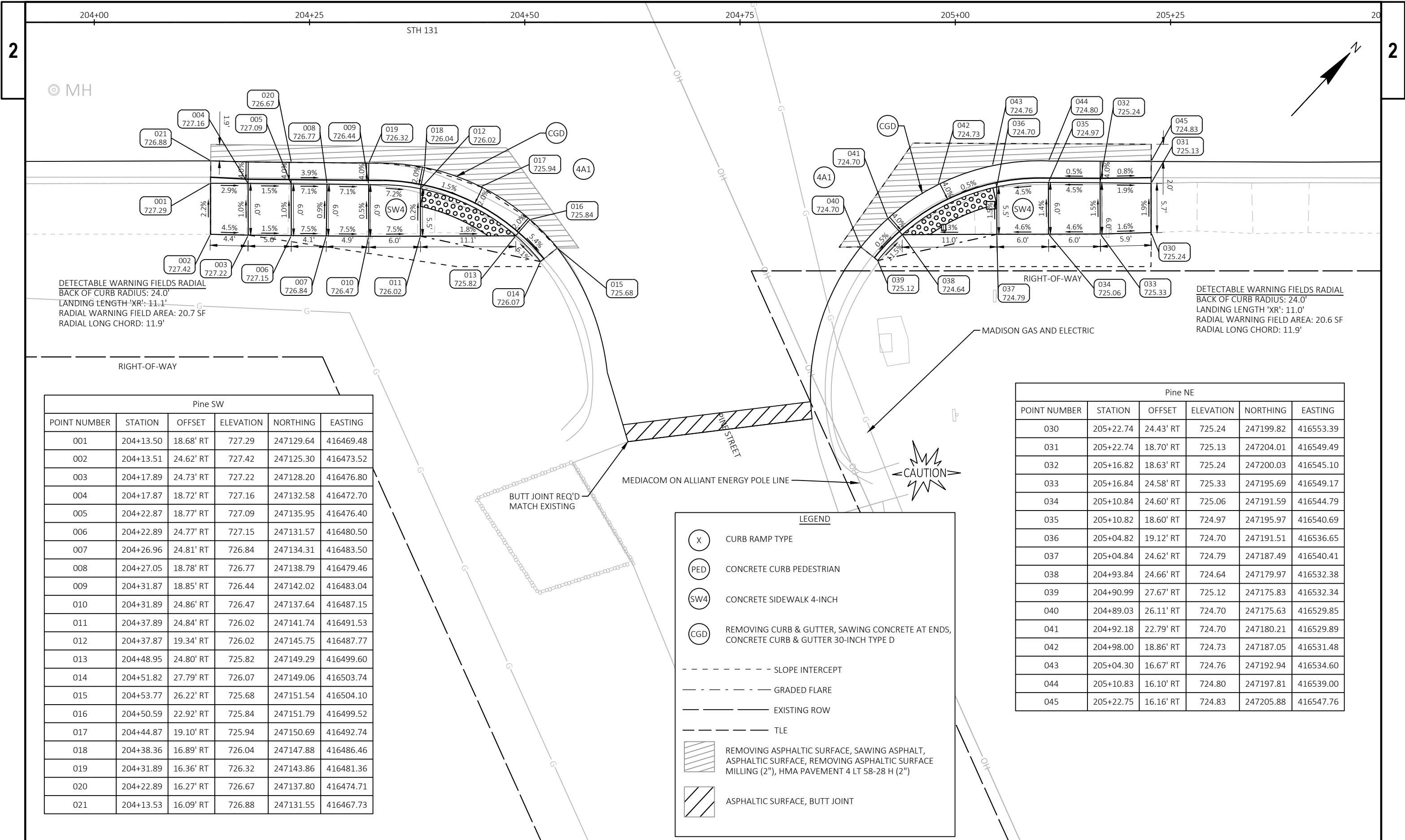
STEP 3: REMOVE ASPHALTIC SURFACE MILLING 2"



STEP 4: CONSTRUCT 2" HMA PAVEMENT 4 LT 58-28 H



ASPHALTIC SURFACE AT CURB RAMP REPLACEMENT DETAIL



DETECTABLE WARNING FIELDS RADIAL
BACK OF CURB RADIUS: 24.0'
LANDING LENGTH 'XR': 11.1'
RADIAL WARNING FIELD AREA: 20.7 SF
RADIAL LONG CHORD: 11.9'

DETECTABLE WARNING FIELDS RADIAL
BACK OF CURB RADIUS: 24.0'
LANDING LENGTH 'XR': 11.0'
RADIAL WARNING FIELD AREA: 20.6 SF
RADIAL LONG CHORD: 11.9'

Pine SW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
001	204+13.50	18.68' RT	727.29	247129.64	416469.48
002	204+13.51	24.62' RT	727.42	247125.30	416473.52
003	204+17.89	24.73' RT	727.22	247128.20	416476.80
004	204+17.87	18.72' RT	727.16	247132.58	416472.70
005	204+22.87	18.77' RT	727.09	247135.95	416476.40
006	204+22.89	24.77' RT	727.15	247131.57	416480.50
007	204+26.96	24.81' RT	726.84	247134.31	416483.50
008	204+27.05	18.78' RT	726.77	247138.79	416479.46
009	204+31.87	18.85' RT	726.44	247142.02	416483.04
010	204+31.89	24.86' RT	726.47	247137.64	416487.15
011	204+37.89	24.84' RT	726.02	247141.74	416491.53
012	204+37.87	19.34' RT	726.02	247145.75	416487.77
013	204+48.95	24.80' RT	725.82	247149.29	416499.60
014	204+51.82	27.79' RT	726.07	247149.06	416503.74
015	204+53.77	26.22' RT	725.68	247151.54	416504.10
016	204+50.59	22.92' RT	725.84	247151.79	416499.52
017	204+44.87	19.10' RT	725.94	247150.69	416492.74
018	204+38.36	16.89' RT	726.04	247147.88	416486.46
019	204+31.89	16.36' RT	726.32	247143.86	416481.36
020	204+22.89	16.27' RT	726.67	247137.80	416474.71
021	204+13.53	16.09' RT	726.88	247131.55	416467.73

Pine NE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
030	205+22.74	24.43' RT	725.24	247199.82	416553.39
031	205+22.74	18.70' RT	725.13	247204.01	416549.49
032	205+16.82	18.63' RT	725.24	247200.03	416545.10
033	205+16.84	24.58' RT	725.33	247195.69	416549.17
034	205+10.84	24.60' RT	725.06	247191.59	416544.79
035	205+10.82	18.60' RT	724.97	247195.97	416540.69
036	205+04.82	19.12' RT	724.70	247191.51	416536.65
037	205+04.84	24.62' RT	724.79	247187.49	416540.41
038	204+93.84	24.66' RT	724.64	247179.97	416532.38
039	204+90.99	27.67' RT	725.12	247175.83	416532.34
040	204+89.03	26.11' RT	724.70	247175.63	416529.85
041	204+92.18	22.79' RT	724.70	247180.21	416529.89
042	204+98.00	18.86' RT	724.73	247187.05	416531.48
043	205+04.30	16.67' RT	724.76	247192.94	416534.60
044	205+10.83	16.10' RT	724.80	247197.81	416539.00
045	205+22.75	16.16' RT	724.83	247205.88	416547.76

LEGEND

X

CURB RAMP TYPE

PED

CONCRETE CURB PEDESTRIAN

SW4

CONCRETE SIDEWALK 4-INCH

CGD

REMOVING CURB & GUTTER, SAWING CONCRETE AT ENDS,
CONCRETE CURB & GUTTER 30-INCH TYPE D

-----SLOPE INTERCEPT

-----GRADED FLARE

-----EXISTING ROW

-----TLE

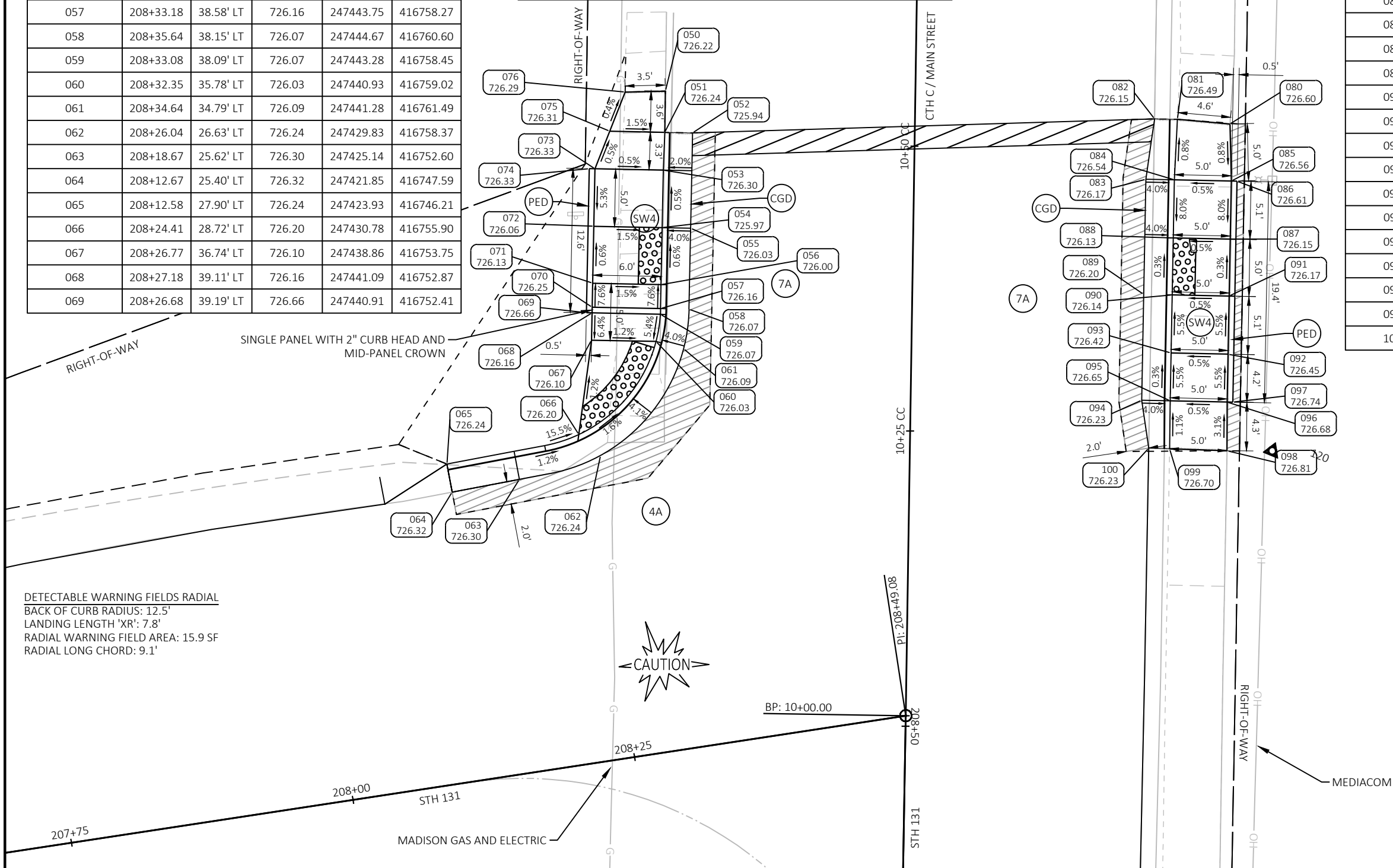
REMOVING ASPHALTIC SURFACE, SAWING ASPHALT,
ASPHALTIC SURFACE, REMOVING ASPHALTIC SURFACE
MILLING (2"), HMA PAVEMENT 4 LT 58-28 H (2")

ASPHALTIC SURFACE, BUTT JOINT

CTH C NW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
050	208+36.47	57.38' LT	726.22	247461.53	416751.33
051	208+35.87	53.81' LT	726.24	247458.17	416752.67
052	208+38.27	53.39' LT	725.94	247459.06	416754.94
053	208+35.24	50.55' LT	726.30	247455.06	416753.83
054	208+34.38	45.57' LT	725.97	247450.36	416755.68
055	208+36.85	45.15' LT	726.03	247451.28	416758.00
056	208+33.53	40.65' LT	726.00	247445.71	416757.50
057	208+33.18	38.58' LT	726.16	247443.75	416758.27
058	208+35.64	38.15' LT	726.07	247444.67	416760.60
059	208+33.08	38.09' LT	726.07	247443.28	416758.45
060	208+32.35	35.78' LT	726.03	247440.93	416759.02
061	208+34.64	34.79' LT	726.09	247441.28	416761.49
062	208+26.04	26.63' LT	726.24	247429.83	416758.37
063	208+18.67	25.62' LT	726.30	247425.14	416752.60
064	208+12.67	25.40' LT	726.32	247421.85	416747.59
065	208+12.58	27.90' LT	726.24	247423.93	416746.21
066	208+24.41	28.72' LT	726.20	247430.78	416755.90
067	208+26.77	36.74' LT	726.10	247438.86	416753.75
068	208+27.18	39.11' LT	726.16	247441.09	416752.87
069	208+26.68	39.19' LT	726.66	247440.91	416752.41

CTH C NW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
070	208+27.26	39.60' LT	726.25	247441.56	416752.69
071	208+27.62	41.67' LT	726.13	247443.51	416751.92
072	208+28.47	46.60' LT	726.06	247448.17	416750.09
073	208+29.33	51.57' LT	726.33	247452.86	416748.24
074	208+28.84	51.66' LT	726.33	247452.68	416747.78
075	208+31.11	54.63' LT	726.31	247456.40	416748.18
076	208+32.97	57.82' LT	726.29	247460.09	416748.11

CTH C NE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
080	10+52.37 CC	27.67' RT	726.60	247477.92	416798.20
081	10+52.71 CC	23.04' RT	726.49	247476.53	416793.77
082	10+52.72 CC	21.04' RT	726.15	247475.79	416791.91
083	10+47.41 CC	20.36' RT	726.17	247470.61	416793.25
084	10+47.39 CC	22.86' RT	726.54	247471.52	416795.58
085	10+47.37 CC	27.86' RT	726.56	247473.35	416800.23
086	10+47.37 CC	28.36' RT	726.61	247473.53	416800.69
087	10+42.27 CC	27.84' RT	726.15	247468.61	416802.10
088	10+42.30 CC	22.84' RT	726.13	247466.78	416797.44
089	10+37.31 CC	20.32' RT	726.20	247461.21	416796.95
090	10+37.30 CC	22.82' RT	726.14	247462.13	416799.28
091	10+37.27 CC	27.82' RT	726.17	247463.96	416803.93
092	10+32.17 CC	27.80' RT	726.45	247459.21	416805.80
093	10+32.19 CC	22.80' RT	726.42	247457.38	416801.15
094	10+28.01 CC	20.28' RT	726.23	247452.56	416800.36
095	10+27.99 CC	22.78' RT	726.65	247453.47	416802.69
096	10+27.97 CC	27.78' RT	726.68	247455.30	416807.34
097	10+27.97 CC	28.28' RT	726.74	247455.49	416807.81
098	10+23.69 CC	27.84' RT	726.81	247451.35	416808.98
099	10+23.78 CC	22.81' RT	726.70	247449.57	416804.27
100	10+23.81 CC	20.97' RT	726.23	247448.92	416802.55



LEGEND

X

CURB RAMP TYPE

PED

CONCRETE CURB PEDESTRIAN

SW4

CONCRETE SIDEWALK 4-INCH

CGD

REMOVING CURB & GUTTER, SAWING CONCRETE AT ENDS,
CONCRETE CURB & GUTTER 30-INCH TYPE D

SLOPE INTERCEPT

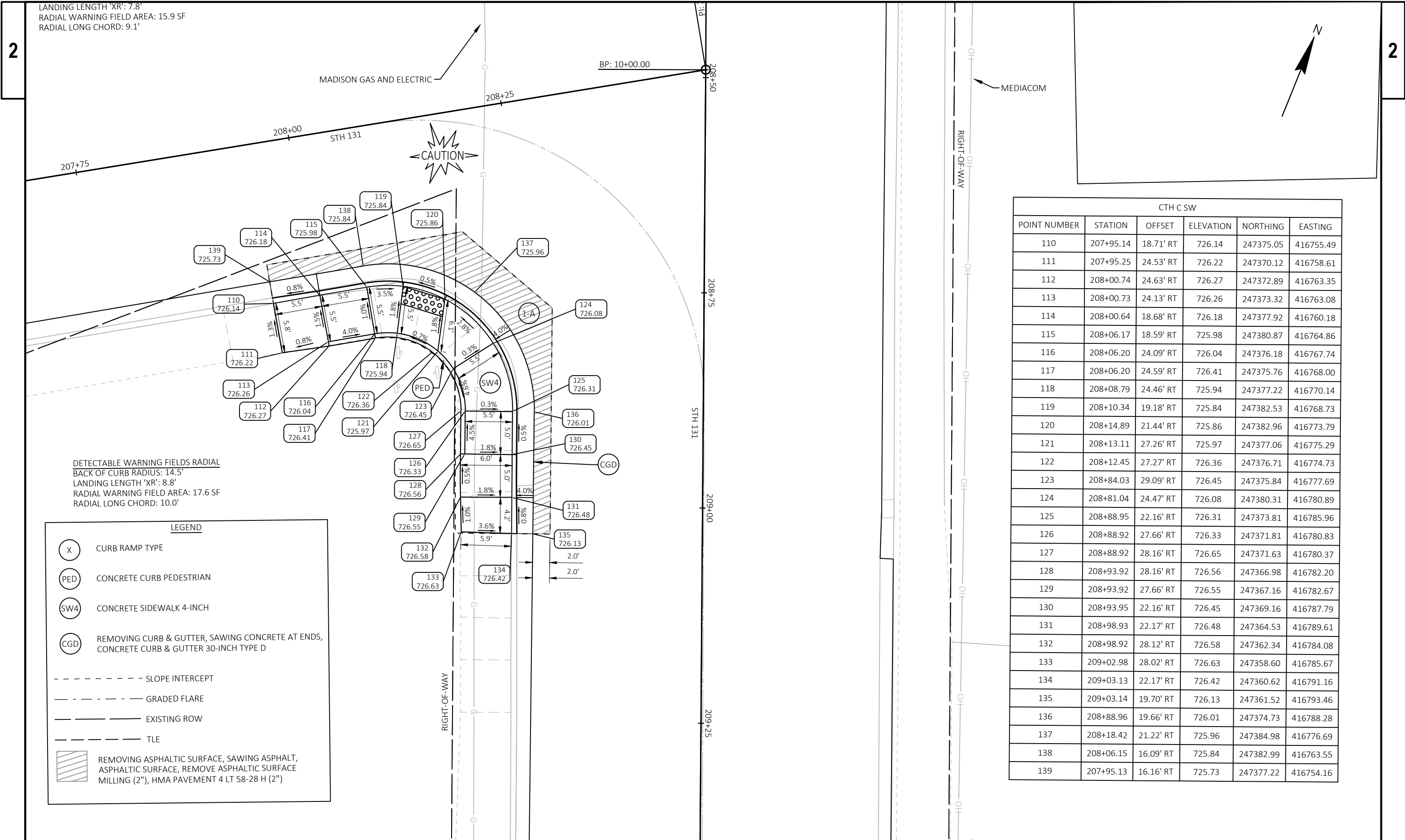
GRADED FLARE

EXISTING ROW

TLE

REMOVING ASPHALTIC SURFACE, SAWING ASPHALT,
ASPHALTIC SURFACE, REMOVING ASPHALTIC SURFACE
MILLING (2"), HMA PAVEMENT 4 LT 58-28 H (2")

ASPHALTIC SURFACE, BUTT JOINT



LANDING LENGTH 'XR': 7.8'
RADIAL WARNING FIELD AREA: 15.9 SF
RADIAL LONG CHORD: 9.1'

MADISON GAS AND ELECTRIC

BP: 10+00.00

MEDIACOM

207+75

208+00

STH 131

CAUTION

208+25

208+50

208+75

209+00

209+25

STH 131







DETECTABLE WARNING FIELDS RADIAL
BACK OF CURB RADIUS: 14.5'
LANDING LENGTH 'XR': 8.8'
RADIAL WARNING FIELD AREA: 17.6 SF
RADIAL LONG CHORD: 10.0'

LEGEND

- (X) CURB RAMP TYPE
- (PED) CONCRETE CURB PEDESTRIAN
- (SW4) CONCRETE SIDEWALK 4-INCH
- (CGD) REMOVING CURB & GUTTER, SAWING CONCRETE AT ENDS,
CONCRETE CURB & GUTTER 30-INCH TYPE D
- SLOPE INTERCEPT
- GRADED FLARE
- EXISTING ROW
- TLE
- REMOVING ASPHALTIC SURFACE, SAWING ASPHALT,
ASPHALTIC SURFACE, REMOVE ASPHALTIC SURFACE
MILLING (2"), HMA PAVEMENT 4 LT 58-28 H (2")

CTH C SW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
110	207+95.14	18.71' RT	726.14	247375.05	416755.49
111	207+95.25	24.53' RT	726.22	247370.12	416758.61
112	208+00.74	24.63' RT	726.27	247372.89	416763.35
113	208+00.73	24.13' RT	726.26	247373.32	416763.08
114	208+00.64	18.68' RT	726.18	247377.92	416760.18
115	208+06.17	18.59' RT	725.98	247380.87	416764.86
116	208+06.20	24.09' RT	726.04	247376.18	416767.74
117	208+06.20	24.59' RT	726.41	247375.76	416768.00
118	208+08.79	24.46' RT	725.94	247377.22	416770.14
119	208+10.34	19.18' RT	725.84	247382.53	416768.73
120	208+14.89	21.44' RT	725.86	247382.96	416773.79
121	208+13.11	27.26' RT	725.97	247377.06	416775.29
122	208+12.45	27.27' RT	726.36	247376.71	416774.73
123	208+84.03	29.09' RT	726.45	247375.84	416777.69
124	208+81.04	24.47' RT	726.08	247380.31	416780.89
125	208+88.95	22.16' RT	726.31	247373.81	416785.96
126	208+88.92	27.66' RT	726.33	247371.81	416780.83
127	208+88.92	28.16' RT	726.65	247371.63	416780.37
128	208+93.92	28.16' RT	726.56	247366.98	416782.20
129	208+93.92	27.66' RT	726.55	247367.16	416782.67
130	208+93.95	22.16' RT	726.45	247369.16	416787.79
131	208+98.93	22.17' RT	726.48	247364.53	416789.61
132	208+98.92	28.12' RT	726.58	247362.34	416784.08
133	209+02.98	28.02' RT	726.63	247358.60	416785.67
134	209+03.13	22.17' RT	726.42	247360.62	416791.16
135	209+03.14	19.70' RT	726.13	247361.52	416793.46
136	208+88.96	19.66' RT	726.01	247374.73	416788.28
137	208+18.42	21.22' RT	725.96	247384.98	416776.69
138	208+06.15	16.09' RT	725.84	247382.99	416763.55
139	207+95.13	16.16' RT	725.73	247377.22	416754.16

3rd St SE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
190	225+81.17	29.03' RT	734.75	246441.21	418105.48
191	225+81.14	25.00' RT	734.69	246444.93	418107.01
192	225+86.14	25.00' RT	734.78	246442.99	418111.62
193	225+86.16	30.00' RT	734.82	246438.37	418109.71
194	225+91.17	30.00' RT	734.89	246436.44	418114.32
195	225+91.14	25.00' RT	734.85	246441.05	418116.23
196	225+95.74	25.00' RT	734.95	246439.27	418120.47
197	225+95.76	28.91' RT	734.95	246435.66	418118.98
198	225+94.12	20.00' RT	735.08	246444.51	418120.91
199	225+91.12	20.00' RT	734.53	246445.67	418118.15
200	225+86.11	20.04' RT	734.45	246447.58	418113.52
201	225+83.14	20.04' RT	734.82	246448.73	418110.78
202	225+83.12	17.53' RT	734.55	246451.05	418111.73
203	226+14.36	17.37' RT	735.02	246439.09	418140.60

LEGEND	
	CURB RAMP TYPE
	CONCRETE CURB PEDESTRIAN
	CONCRETE SIDEWALK 4-INCH
	REMOVING CURB & GUTTER, SAWING CONCRETE AT ENDS, CONCRETE CURB & GUTTER 30-INCH TYPE D
---	SLOPE INTERCEPT
- - - - -	GRADED FLARE
=====	EXISTING ROW
- - - - -	TLE
	REMOVING ASPHALTIC SURFACE, SAWING ASPHALT, ASPHALTIC SURFACE, REMOVE ASPHALTIC SURFACE MILLING (2"), HMA PAVEMENT 4 LT 58-28 H (2")
	ASPHALTIC SURFACE, BUTT JOINT



61 N					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
210	229+03.62	31.85' LT	742.08	246372.38	418426.33
211	229+03.59	27.93' LT	742.07	246368.78	418424.78
212	229+08.25	26.98' LT	741.89	246366.10	418428.70
213	229+08.21	32.01' LT	741.95	246370.75	418430.62
214	229+13.21	32.01' LT	741.76	246368.82	418435.23
215	229+13.25	27.01' LT	741.69	246364.19	418433.33
216	229+18.26	27.02' LT	741.49	246362.26	418437.95
217	229+18.20	32.02' LT	741.56	246366.89	418439.84
218	229+23.21	32.03' LT	741.36	246364.95	418444.46
219	229+23.26	27.03' LT	741.30	246360.33	418442.56
220	229+23.29	22.53' LT	741.00	246356.16	418440.85
221	229+20.29	22.51' LT	741.37	246357.31	418438.08
222	229+17.32	20.01' LT	740.75	246356.16	418434.37
223	229+31.69	20.59' LT	741.34	246351.13	418447.84
224	229+31.29	23.03' LT	741.69	246353.53	418448.42
225	229+28.29	22.82' LT	741.09	246354.50	418445.57
226	229+28.26	27.03' LT	741.37	246358.39	418447.17
227	229+28.21	32.03' LT	741.43	246363.02	418449.07
228	229+28.21	32.53' LT	742.43	246363.48	418449.26
229	229+13.20	32.51' LT	742.26	246369.28	418435.42

+25

228+50

228+75

229+00

229+25

229+50

STH 131

61 S					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
230	229+14.39	26.13' RT	741.06	246314.76	418413.79
231	229+14.34	22.24' RT	741.06	246318.36	418415.25
232	229+18.67	22.19' RT	741.15	246316.73	418419.26
233	229+18.73	27.19' RT	741.17	246312.10	418417.37
234	229+23.68	22.13' RT	741.25	246314.84	418423.90
235	229+23.73	27.13' RT	741.29	246310.22	418422.01
236	229+28.73	27.07' RT	741.38	246308.34	418426.64
237	229+33.52	27.02' RT	741.65	246306.46	418431.26
238	229+38.23	27.10' RT	741.97	246304.30	418436.57
239	229+42.45	26.22' RT	742.26	246303.44	418441.69
240	229+42.64	22.35' RT	742.27	246307.04	418443.11
241	229+38.40	22.10' RT	741.97	246308.92	418438.48
242	229+33.54	22.01' RT	741.64	246311.07	418433.18
243	229+28.67	22.07' RT	741.34	246312.97	418428.52
244	229+28.65	18.44' RT	741.10	246316.32	418429.91
245	229+31.65	18.46' RT	741.70	246315.14	418432.67
246	229+31.60	15.91' RT	741.40	246317.51	418433.61
247	229+20.62	16.04' RT	741.01	246321.65	418423.44
248	229+20.65	18.54' RT	741.29	246319.33	418422.50
249	229+23.65	18.50' RT	741.01	246318.20	418425.28

LEGEND

X

CURB RAMP TYPE

PED

CONCRETE CURB PEDESTRIAN

SW4

CONCRETE SIDEWALK 4-INCH

CGD

REMOVING CURB & GUTTER, SAWING CONCRETE AT ENDS,
CONCRETE CURB & GUTTER 30-INCH TYPE D

SLOPE INTERCEPT

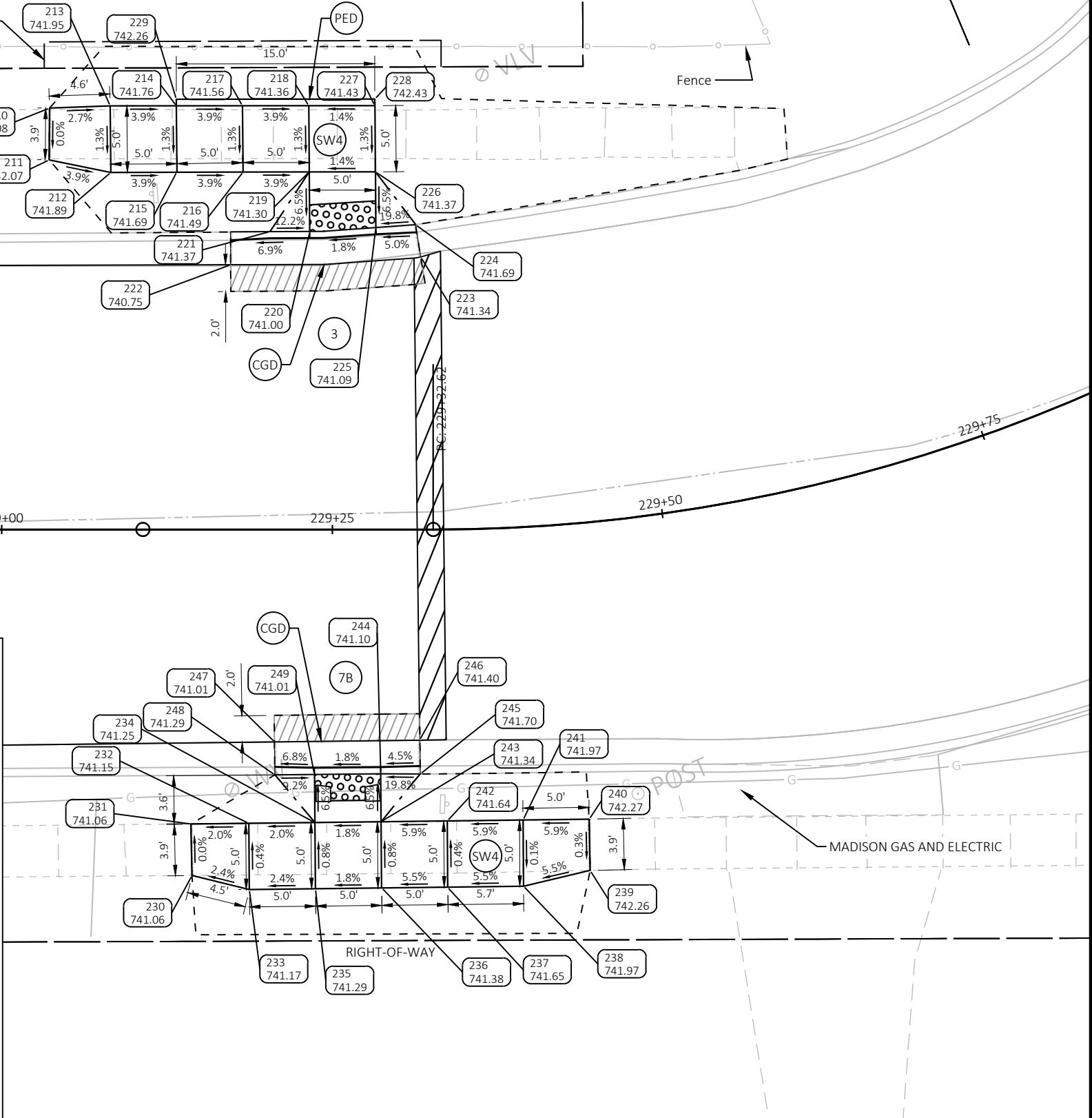
GRADED FLARE

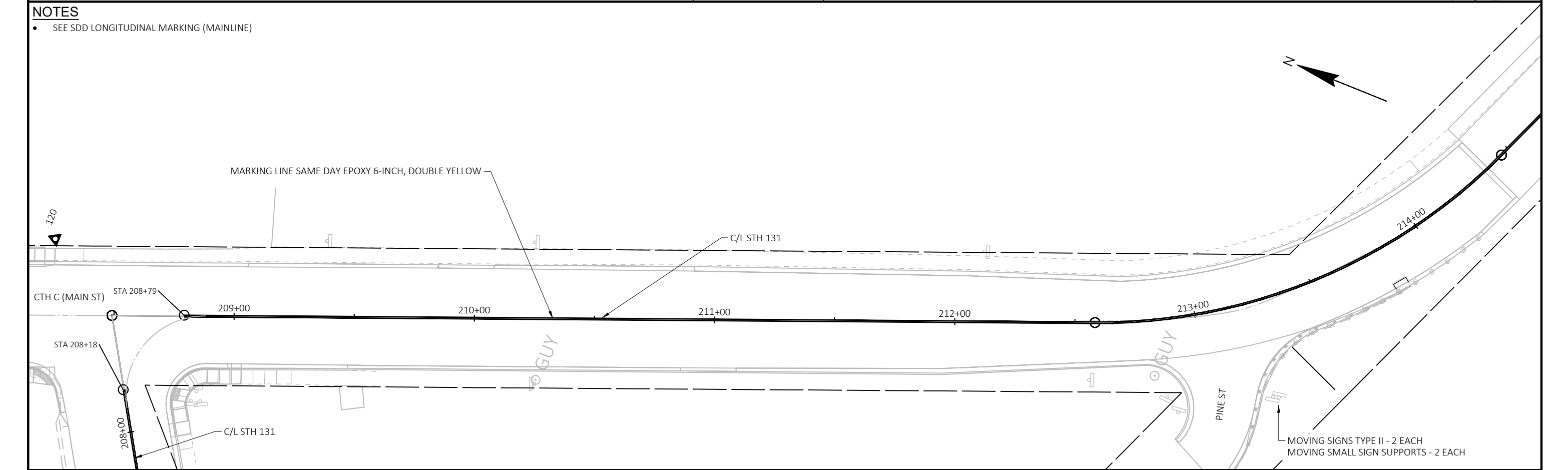
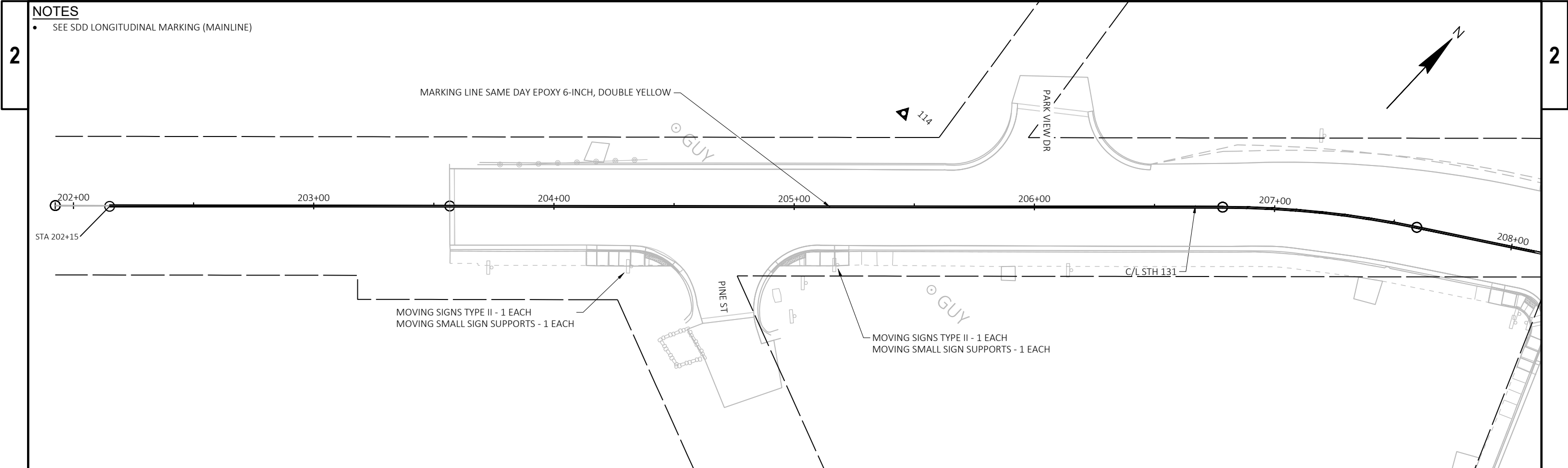
EXISTING ROW

TLE

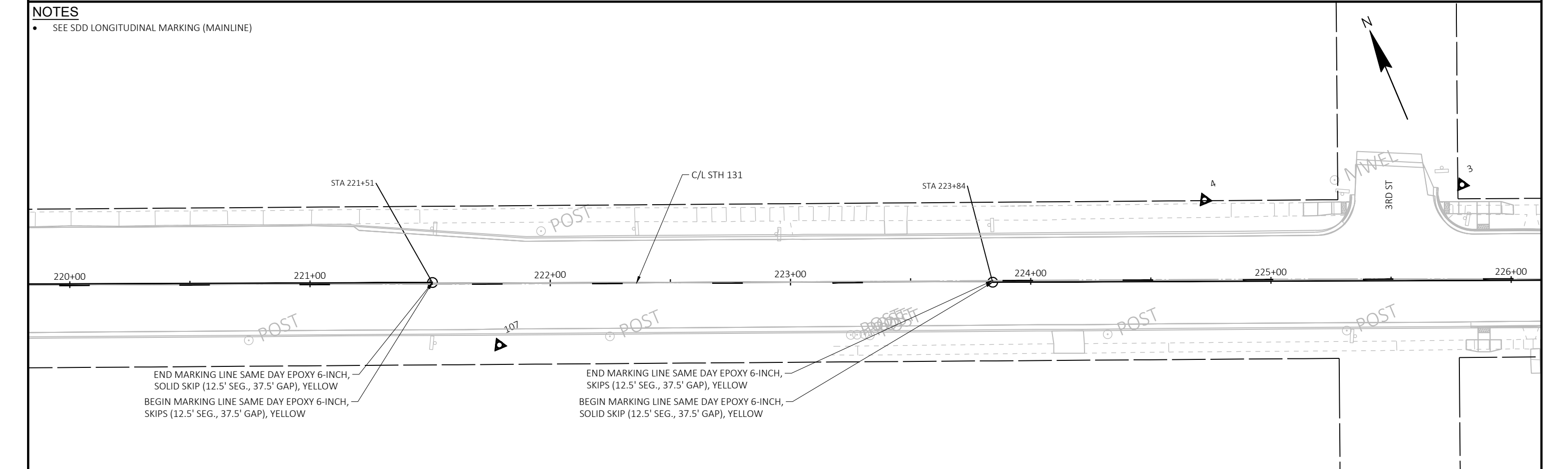
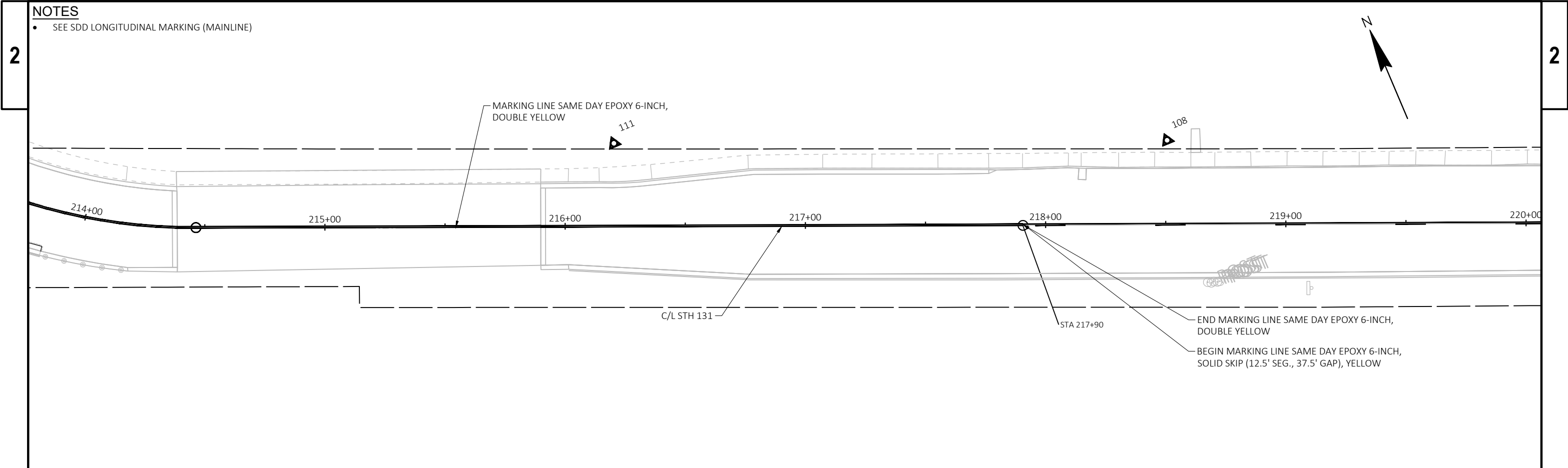
REMOVING ASPHALTIC SURFACE, SAWING ASPHALT,
ASPHALTIC SURFACE, REMOVE SURFACE MILLING (2"),
HMA PAVEMENT 4 LT 58-28 H (2")

ASPHALTIC SURFACE, BUTT JOINT

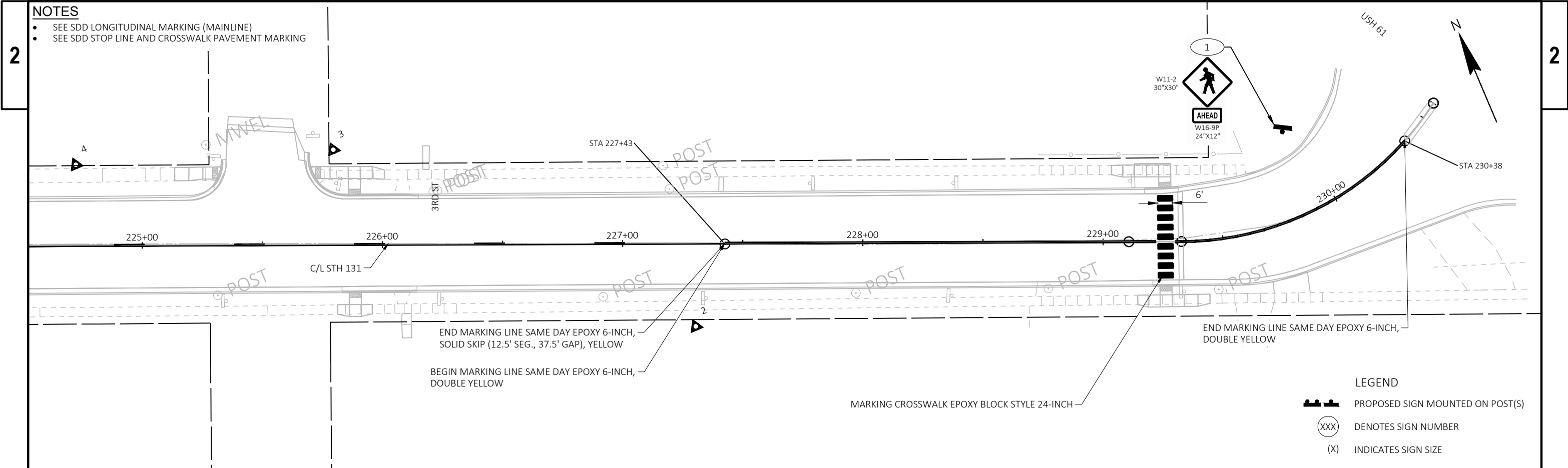


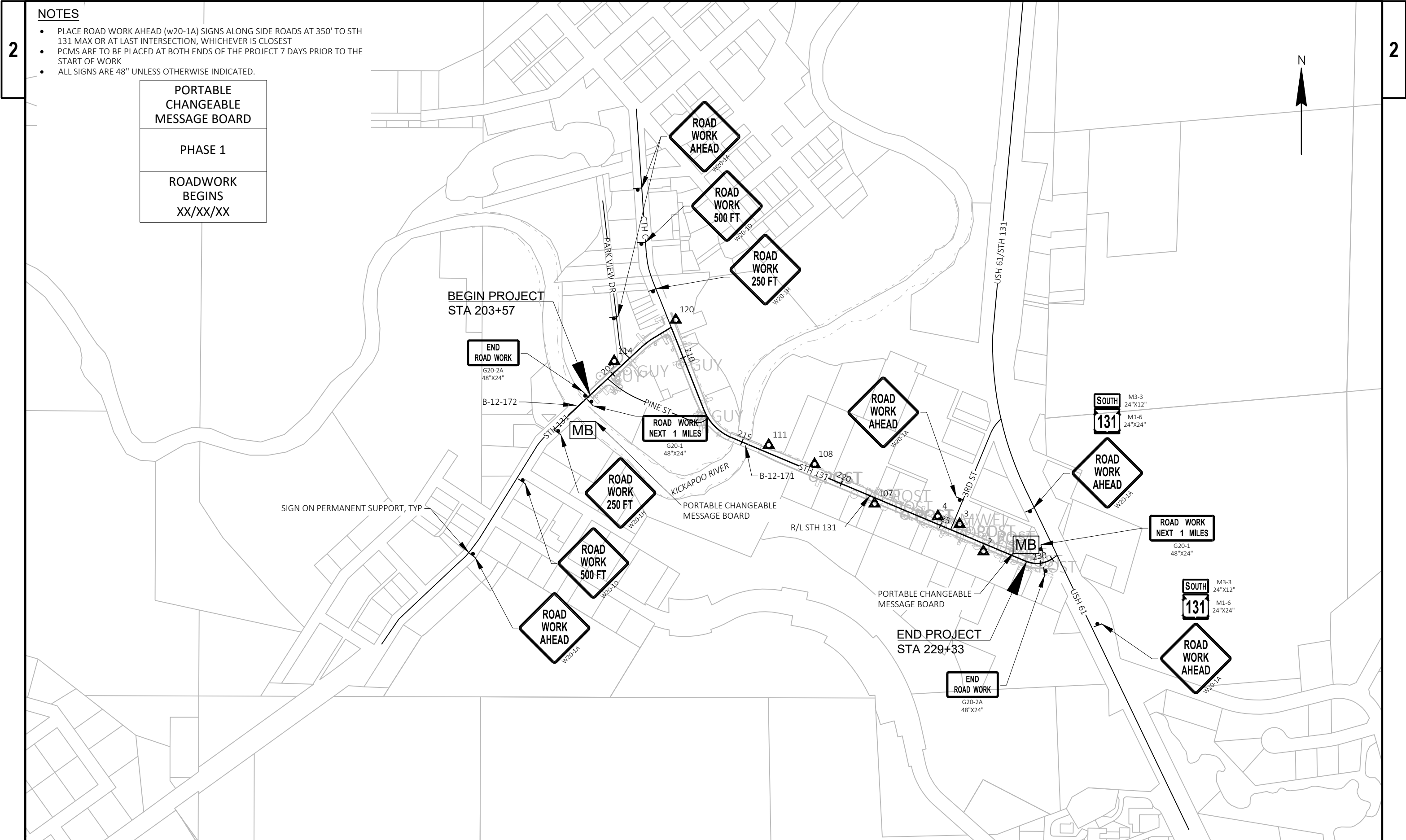


PROJECT NO: 5780-03-63	HWY: STH 131	COUNTY: CRAWFORD	PAVEMENT MARKING AND SIGNING DETAILS	SHEET	E
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PROJECT NO: 5780-03-63	HWY: STH 131	COUNTY: CRAWFORD	PAVEMENT MARKING AND SIGNING DETAILS	SHEET	E
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NOTES

- PLACE ROAD WORK AHEAD (w20-1A) SIGNS ALONG SIDE ROADS AT 350' TO STH 131 MAX OR AT LAST INTERSECTION, WHICHEVER IS CLOSEST
- PCMS ARE TO BE PLACED AT BOTH ENDS OF THE PROJECT 7 DAYS PRIOR TO THE START OF WORK
- ALL SIGNS ARE 48" UNLESS OTHERWISE INDICATED.

PORTABLE CHANGEABLE MESSAGE BOARD
PHASE 1
ROADWORK BEGINS XX/XX/XX

PROJECT NO: 5780-03-63

HWY: STH 131

COUNTY: CRAWFORD

ADVANCE WARNING SIGNING

SHEET

E

FILE NAME : W:\57800363\C3D\SHEETS\025000-AW.DWG
LAYOUT NAME - 01-aw

PLOT DATE : 4/16/2025 3:24 PM












PLOT BY : CORONA WOYCHIK

PLOT NAME :

PLOT SCALE : 1 IN=400 FT

WISDOT/CADDs SHEET 42

LEGEND

	WORK AREA
	TEMPORARY CURB RAMP
	TEMPORARY PEDESTRIAN SURFACE
	TEMPORARY PEDESTRIAN BARRICADE
	TEMPORARY DETECTABLE WARNING FIELD
	TRAFFIC CONTROL DRUM
	SIGN ON PERMANENT SUPPORT
	SIGN ON TEMPORARY SUPPORT
	DIRECTION OF TRAFFIC
	WHITE 6" TEMPORARY MARKING CROSSWALK PAINT
	DOUBLE YELLOW 6" TEMPORARY MARKING LINE REMOVABLE TAPE

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING, OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS, 36"X36" SIGNS MAY BE USED INSTEAD OF 48"X48" SIGNS.

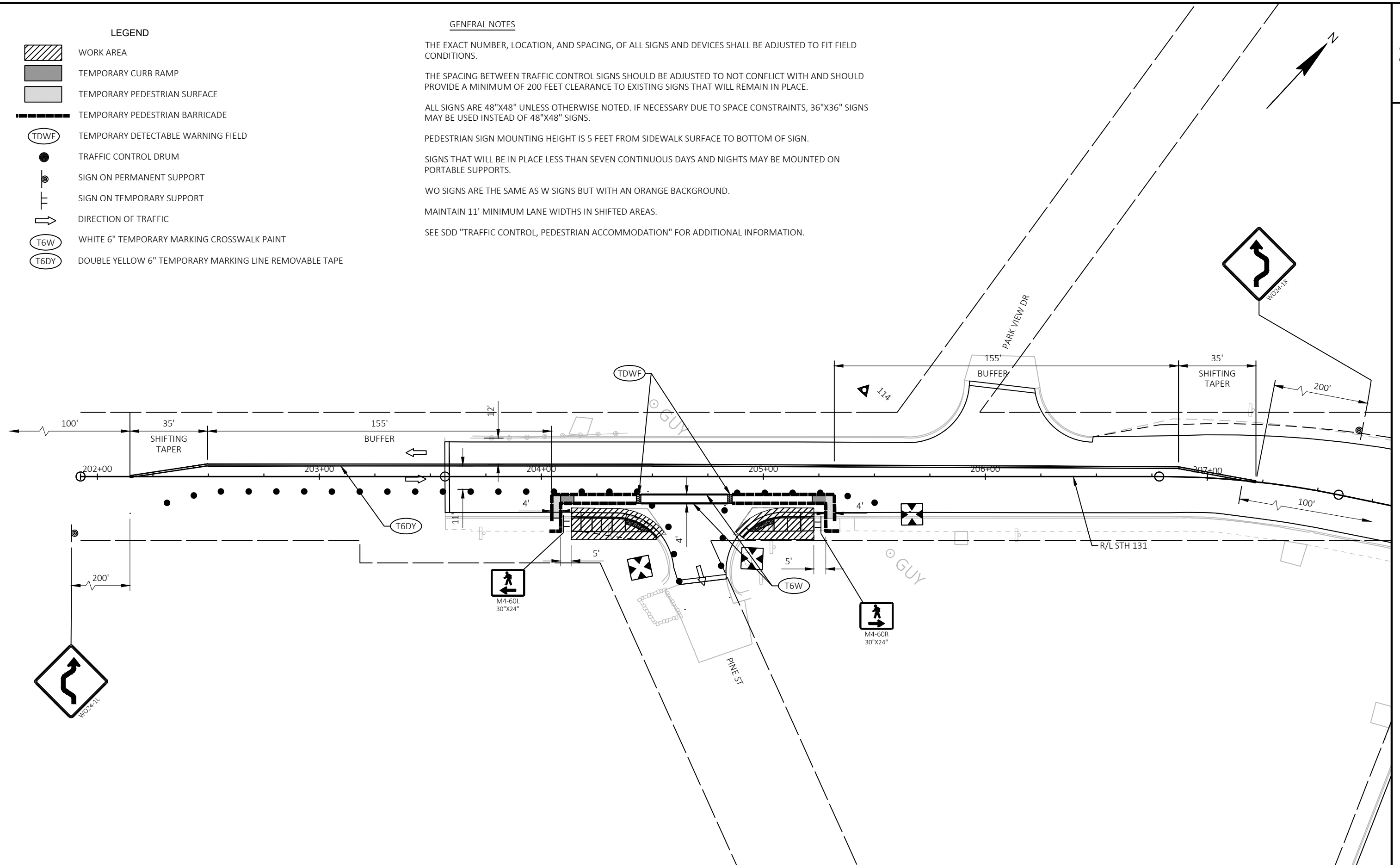
PEDESTRIAN SIGN MOUNTING HEIGHT IS 5 FEET FROM SIDEWALK SURFACE TO BOTTOM OF SIGN.

SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.








WO SIGNS ARE THE SAME AS W SIGNS BUT WITH AN ORANGE BACKGROUND.

MAINTAIN 11' MINIMUM LANE WIDTHS IN SHIFTED AREAS.

SEE SDD "TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION" FOR ADDITIONAL INFORMATION.



LEGEND

	WORK AREA
	TEMPORARY CURB RAMP
	TEMPORARY PEDESTRIAN BARRICADE
	TRAFFIC CONTROL DRUM
	SIGN ON TEMPORARY SUPPORT
	TEMPORARY MARKING CROSSWALK REMOVABLE TAPE 6 - INCH
	TEMPORARY MARKING CROSSWALK PAINT 6-INCH

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING, OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS, 36"X36" SIGNS MAY BE USED INSTEAD OF 48"X48" SIGNS.

PEDESTRIAN SIGN MOUNTING HEIGHT IS 5 FEET FROM SIDEWALK SURFACE TO BOTTOM OF SIGN.

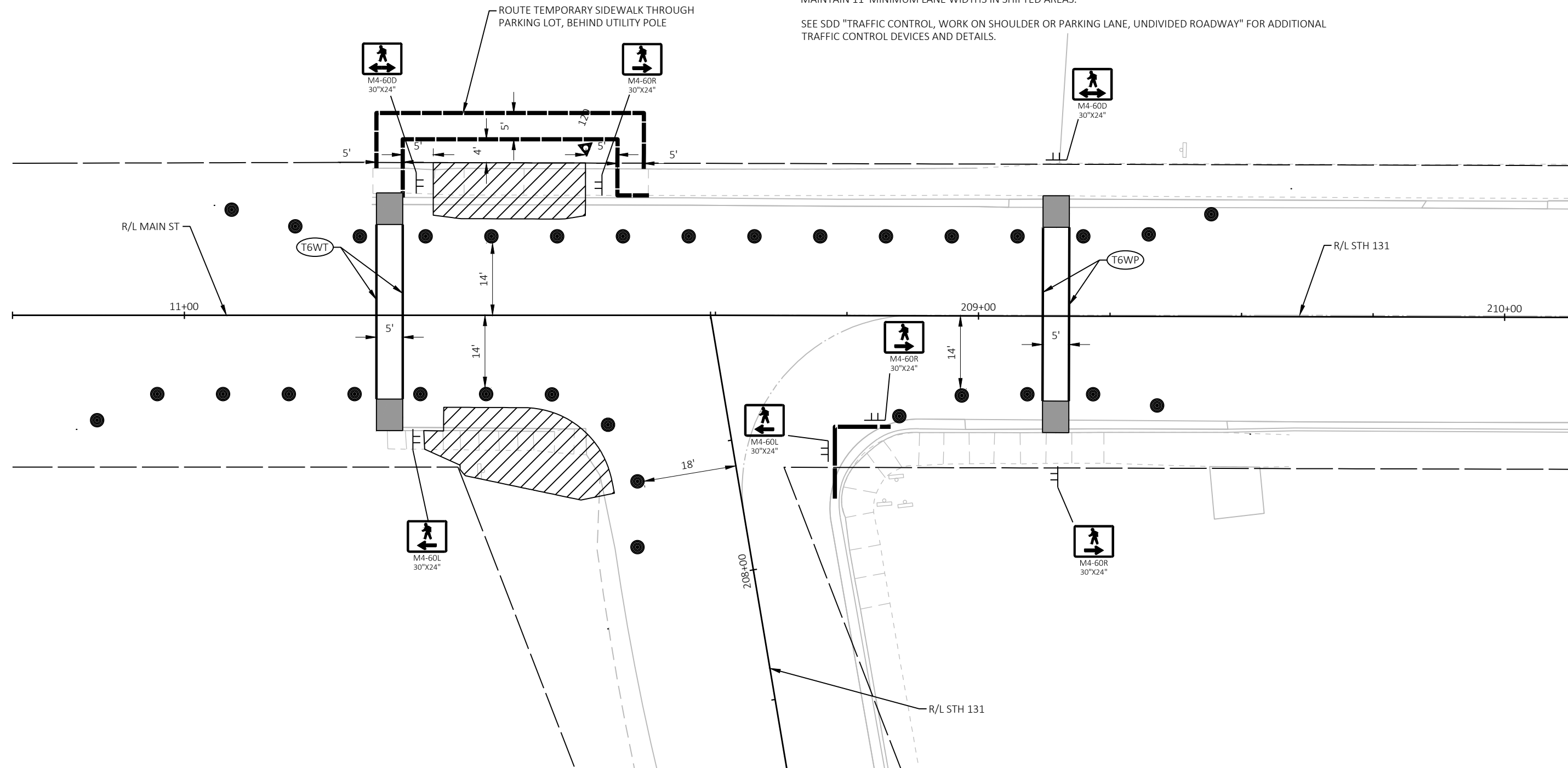
SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

WO SIGNS ARE THE SAME AS W SIGNS BUT WITH AN ORANGE BACKGROUND.

MAINTAIN 11' MINIMUM LANE WIDTHS IN SHIFTED AREAS.

SEE SDD "TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY" FOR ADDITIONAL TRAFFIC CONTROL DEVICES AND DETAILS.

SEE SDD "TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATIONS"



PROJECT NO: 5780-03-63

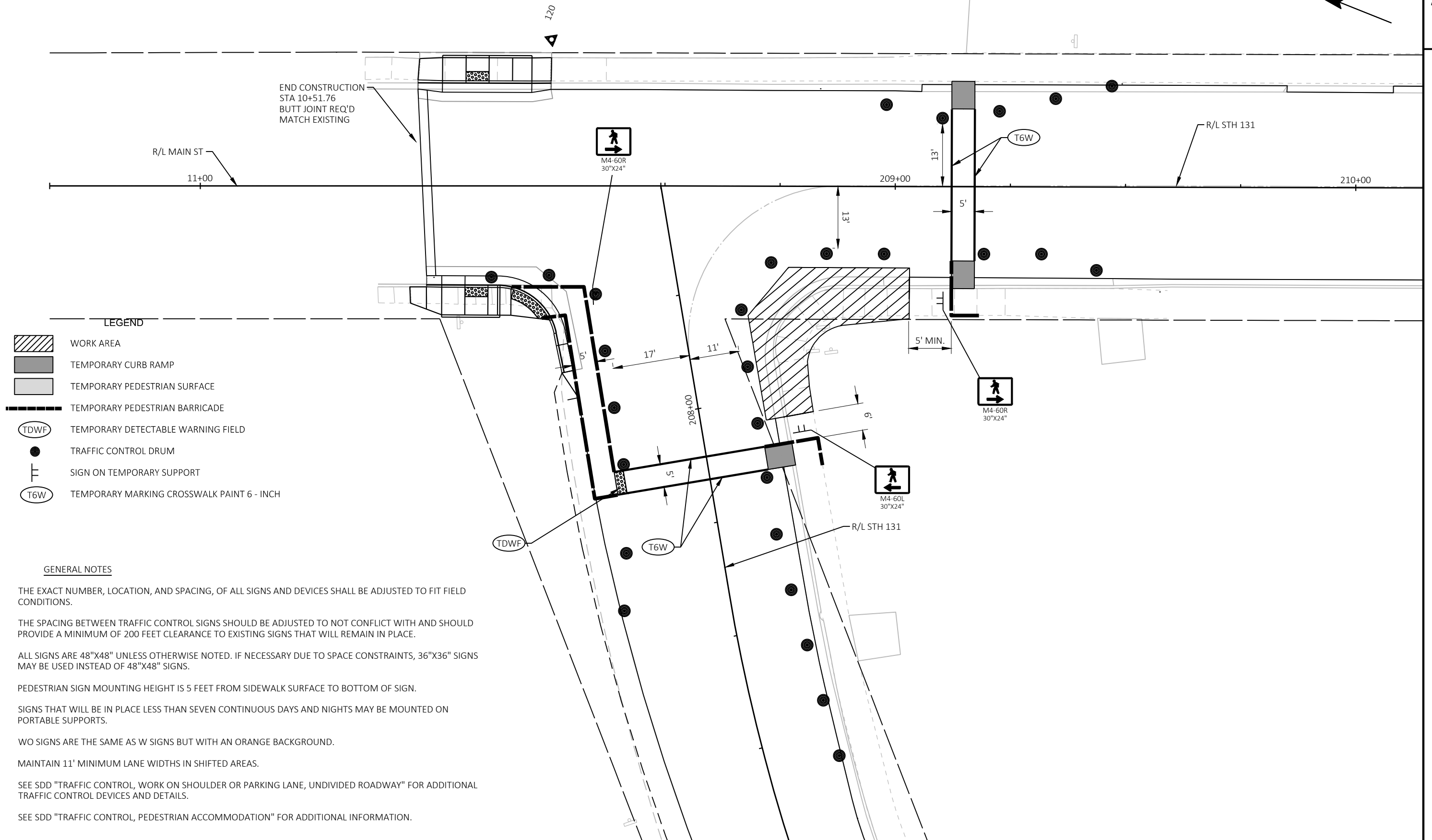
HWY: STH 131

COUNTY: CRAWFORD








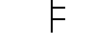
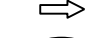
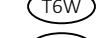
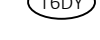
TRAFFIC CONTROL PEDESTRIAN ACCOMMODATIONS - CTH C STAGE 1

SHEET

E



LEGEND

	WORK AREA
	TEMPORARY CURB RAMP
	TEMPORARY PEDESTRIAN SURFACE
	TEMPORARY PEDESTRIAN BARRICADE
	TEMPORARY DETECTABLE WARNING FIELD
	TRAFFIC CONTROL DRUM
	SIGN ON PERMANENT SUPPORT
	SIGN ON TEMPORARY SUPPORT
	DIRECTION OF TRAFFIC
	TEMPORARY MARKING LINE CROSSWALK PAINT 6-INCH, WHITE
	DOUBLE YELLOW 6" TEMPORARY MARKING LINE PAINT

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING, OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS, 36"X36" SIGNS MAY BE USED INSTEAD OF 48"X48" SIGNS.

PEDESTRIAN SIGN MOUNTING HEIGHT IS 5 FEET FROM SIDEWALK SURFACE TO BOTTOM OF SIGN.

SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

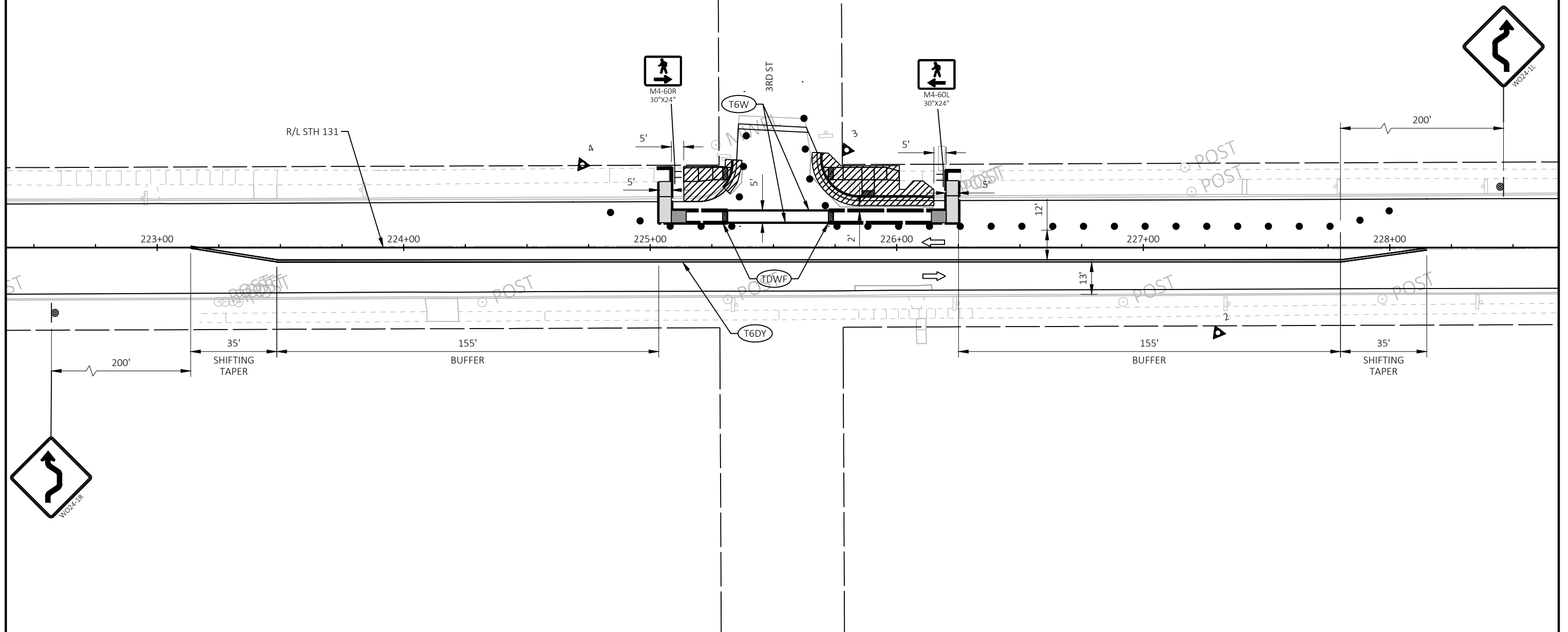
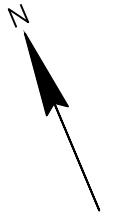
WO SIGNS ARE THE SAME AS W SIGNS BUT WITH AN ORANGE BACKGROUND.

MAINTAIN 11' MINIMUM LANE WIDTHS IN SHIFTED AREAS.

SEE SDD "TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION" FOR ADDITIONAL INFORMATION.

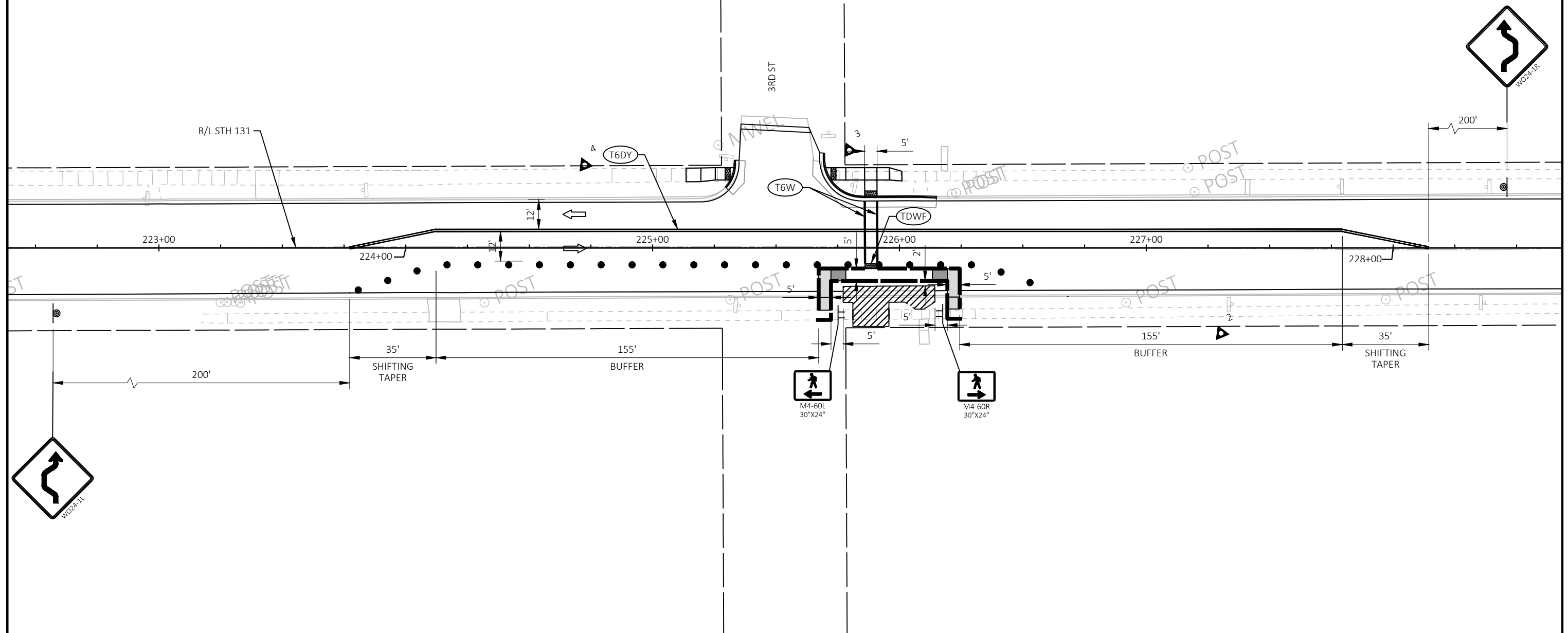
STAGING

DO NOT CONSTRUCT CURB RAMPS IN NW AND SW QUADRANT OF 3RD ST (STAGE 1) AT SAME TIME OF CONSTRUCTING CURB RAMP ON SOUTH SIDE OF STH 131 AT USH 61.












STAGING

- SOUTHEAST CURB RAMP AT 3RD ST (STAGE 2) CAN BE CONSTRUCTED CONCURRENTLY WITH RAMP ON SOUTH SIDE OF STH 131 AT USH 61 IF LANE SHIFT IS CARRIED OUT BETWEEN BOTH WORK AREAS.



LEGEND

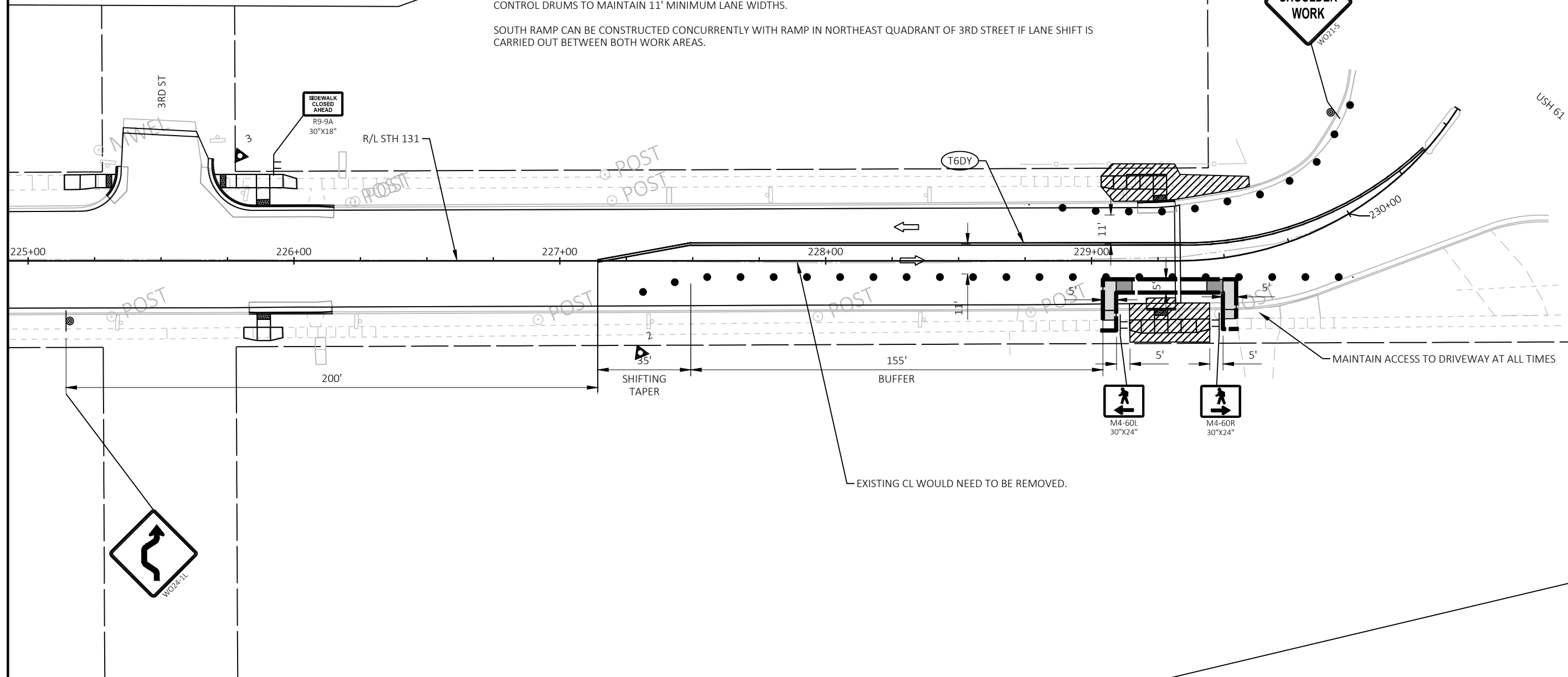
-  WORK AREA
-  TEMPORARY CURB RAMP
-  TEMPORARY PEDESTRIAN SURFACE
-  TEMPORARY PEDESTRIAN BARRICADE
-  TRAFFIC CONTROL DRUM
-  SIGN ON PERMANENT SUPPORT
-  SIGN ON TEMPORARY SUPPORT
-  DIRECTION OF TRAFFIC
-  DOUBLE YELLOW 6" TEMPORARY MARKING LINE REMOVABLE TAPE

GENERAL NOTES

- THE EXACT NUMBER, LOCATION, AND SPACING, OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.
- THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.
- ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS, 36"X36" SIGNS MAY BE USED INSTEAD OF 48"X48" SIGNS
- PEDESTRIAN SIGN MOUNTING HEIGHT IS 5 FEET FROM SIDEWALK SURFACE TO BOTTOM OF SIGN.
- SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.
- WO SIGNS ARE THE SAME AS W SIGNS BUT WITH AN ORANGE BACKGROUND.
- MAINTAIN 11' MINIMUM LANE WIDTHS IN SHIFTED AREAS.
- SEE SDD "TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION" FOR ADDITIONAL INFORMATION.

STAGING

- IF NORTH RAMP IS CONSTRUCTED CONCURRENTLY WITH SOUTH RAMP, USE FLEXIBLE TUBULAR MARKERS IN PLACE OF TRAFFIC CONTROL DRUMS TO MAINTAIN 11' MINIMUM LANE WIDTHS.
- SOUTH RAMP CAN BE CONSTRUCTED CONCURRENTLY WITH RAMP IN NORTHEAST QUADRANT OF 3RD STREET IF LANE SHIFT IS CARRIED OUT BETWEEN BOTH WORK AREAS.



Estimate Of Quantities

5780-03-63

Line	Item	Item Description	Unit	Total	Qty
0002	204.0110	Removing Asphaltic Surface	SY	104.000	104.000
0004	204.0115	Removing Asphaltic Surface Butt Joints	SY	67.000	67.000
0006	204.0120	Removing Asphaltic Surface Milling	SY	10,611.000	10,611.000
0008	204.0150	Removing Curb & Gutter	LF	425.000	425.000
0010	204.0155	Removing Concrete Sidewalk	SY	166.000	166.000
0012	204.0165	Removing Guardrail	LF	69.000	69.000
0014	211.0101	Prepare Foundation for Asphaltic Paving (project) 01. 5780-03-63	EACH	1.000	1.000
0016	213.0100	Finishing Roadway (project) 01. 5780-03-63	EACH	1.000	1.000
0018	305.0110	Base Aggregate Dense 3/4-Inch	TON	20.000	20.000
0020	305.0500	Shaping Shoulders	STA	2.000	2.000
0022	450.4000	HMA Cold Weather Paving	TON	1,412.000	1,412.000
0024	455.0605	Tack Coat	GAL	884.000	884.000
0026	460.2000	Incentive Density HMA Pavement	DOL	770.000	770.000
0028	460.5424	HMA Pavement 4 LT 58-28 H	TON	1,412.000	1,412.000
0030	465.0105	Asphaltic Surface	TON	95.000	95.000
0032	465.0110	Asphaltic Surface Patching	TON	5.000	5.000
0034	601.0411	Concrete Curb & Gutter 30-Inch Type D	LF	329.000	329.000
0036	601.0600	Concrete Curb Pedestrian	LF	74.000	74.000
0038	602.0405	Concrete Sidewalk 4-Inch	SF	1,554.000	1,554.000
0040	602.0505	Curb Ramp Detectable Warning Field Yellow	SF	90.000	90.000
0042	602.0605	Curb Ramp Detectable Warning Field Radial Yellow	SF	60.000	60.000
0044	611.8110	Adjusting Manhole Covers	EACH	11.000	11.000
0046	614.0010	Barrier System Grading Shaping Finishing	EACH	1.000	1.000
0048	614.0305	Steel Plate Beam Guard Class A	LF	38.000	38.000
0050	614.0345	Steel Plate Beam Guard Short Radius	LF	50.000	50.000
0052	614.0390	Steel Plate Beam Guard Short Radius Terminal	EACH	1.000	1.000
0054	619.1000	Mobilization	EACH	1.000	1.000
0056	624.0100	Water	MGAL	0.400	0.400
0058	625.0500	Salvaged Topsoil	SY	239.000	239.000
0060	628.1504	Silt Fence	LF	192.000	192.000
0062	628.1520	Silt Fence Maintenance	LF	192.000	192.000
0064	628.1905	Mobilizations Erosion Control	EACH	2.000	2.000
0066	628.1910	Mobilizations Emergency Erosion Control	EACH	2.000	2.000
0068	628.2006	Erosion Mat Urban Class I Type A	SY	239.000	239.000
0070	628.7005	Inlet Protection Type A	EACH	2.000	2.000
0072	628.7010	Inlet Protection Type B	EACH	2.000	2.000
0074	628.7015	Inlet Protection Type C	EACH	5.000	5.000
0076	628.7570	Rock Bags	EACH	3.000	3.000
0078	629.0210	Fertilizer Type B	CWT	0.130	0.130
0080	630.0140	Seeding Mixture No. 40	LB	11.000	11.000
0082	630.0500	Seed Water	MGAL	4.000	4.000
0084	634.0614	Posts Wood 4x6-Inch X 14-FT	EACH	1.000	1.000
0086	637.2210	Signs Type II Reflective H	SF	8.250	8.250
0088	638.2102	Moving Signs Type II	EACH	4.000	4.000
0090	638.4000	Moving Small Sign Supports	EACH	4.000	4.000
0092	642.5001	Field Office Type B	EACH	1.000	1.000
0094	643.0300	Traffic Control Drums	DAY	1,158.000	1,158.000
0096	643.0900	Traffic Control Signs	DAY	1,088.000	1,088.000
0098	643.1050	Traffic Control Signs PCMS	DAY	14.000	14.000

Estimate Of Quantities

5780-03-63

Line	Item	Item Description	Unit	Total	Qty
0100	643.3165	Temporary Marking Line Paint 6-Inch	LF	5,801.000	5,801.000
0102	643.3180	Temporary Marking Line Removable Tape 6-Inch	LF	1,657.000	1,657.000
0104	643.3305	Temporary Marking Crosswalk Paint 6-inch	LF	344.000	344.000
0106	643.3350	Temporary Marking Crosswalk Removable Tape 6-inch	LF	66.000	66.000
0108	643.5000	Traffic Control	EACH	1.000	1.000
0110	644.1430	Temporary Pedestrian Surface Plate	SF	550.000	550.000
0112	644.1601	Temporary Pedestrian Curb Ramp	DAY	78.000	78.000
0114	644.1605	Temporary Pedestrian Detectable Warning Field	SF	56.000	56.000
0116	644.1810	Temporary Pedestrian Barricade	LF	1,145.000	1,145.000
0118	646.4720	Marking Line Same Day Epoxy 6-Inch	LF	4,578.000	4,578.000
0120	646.6466	Cold Weather Marking Epoxy 6-Inch	LF	4,578.000	4,578.000
0122	646.7520	Marking Crosswalk Epoxy Block Style 24-Inch	LF	54.000	54.000
0124	646.9000	Marking Removal Line 4-Inch	LF	3,542.000	3,542.000
0126	650.8000	Construction Staking Resurfacing Reference	LF	2,424.000	2,424.000
0128	650.9000	Construction Staking Curb Ramps	EACH	12.000	12.000
0130	650.9911	Construction Staking Supplemental Control (project) 01. 5780-03-63	EACH	1.000	1.000
0132	690.0150	Sawing Asphalt	LF	439.000	439.000
0134	690.0250	Sawing Concrete	LF	111.000	111.000
0136	SPV.0060	Special 01. Adjusting Water Valve Boxes	EACH	5.000	5.000
0138	SPV.0060	Special 02. Adjusting Environmental Monitoring Cap	EACH	1.000	1.000
0140	SPV.0180	Special 01. Removing Distressed Pavement Milling	SY	500.000	500.000

204 REMOVALS										
CATEGORY	STATION	TO	STATION	LOCATION	204.0110	204.0115	204.0120	204.0150	204.0155	REMARKS
					REMOVING ASPHALTIC SURFACE SY	REMOVING ASPHALTIC SURFACE BUTT JOINTS SY	REMOVING ASPHALTIC SURFACE MILLING SY	REMOVING CURB & GUTTER LF	REMOVING CONCRETE SIDEWALK SY	
0010	204+40	-	-	STH 131 AND PINE ST - SW QUADRANT	14	-	-	41	20	
0010	205+00	-	-	STH 131 AND PINE ST - SE QUADRANT	13	-	-	35	16	
0010	208+30	-	-	STH 131 AND CTH C - NW QUADRANT	13	-	-	21	13	
0010	10+40 CC	-	-	STH 131 AND CTH C - NE QUADRANT	7	-	-	29	16	
0010	208+80	-	-	STH 131 AND CTH C - SW QUADRANT	20	-	-	51	28	
0010	225+30	-	-	STH 131 AND 3RD ST - NW QUADRANT	5	-	-	12	11	
0010	225+90	-	-	STH 131 AND 3RD ST - NE QUADRANT	18	-	-	57	17	
0010	225+90	-	-	STH 131 AND 3RD ST - SE QUADRANT	7	-	-	31	9	
0010	229+25	-	-	STH 131 AND USH 61 - NW QUADRANT	4	-	-	14	23	
0010	229+25	-	-	STH 131 AND USH 61 - SW QUADRANT	3	-	-	11	13	
0010	203+57	-	203+59	STH 131	-	7	-	-	-	STH 131, BEGIN OF PROJECT
0010	204+52	-	204+91	RT	-	8	-	-	-	PINE ST
0010	205+68	-	206+48	LT	-	15	-	-	-	PARK VIEW DR
0010	208+38	-	208+49	LT	-	9	-	-	-	CTH C
0010	212+98	-	213+86	RT	-	-	-	123	-	PINE ST
0010	214+36	-	214+38	STH 131	-	7	-	-	-	STH 131, B-12-171
0010	215+90	-	215+92	STH 131	-	7	-	-	-	STH 131, B-12-171
0010	225+36	-	225+64	LT	-	6	-	-	-	3RD ST
0010	229+31	-	229+33	STH 131	-	8	-	-	-	STH 131, END OF PROJECT
0010	203+59	-	214+38	STH 131	-	-	4,737	-	-	
0010	215+90	-	229+31	STH 131	-	-	5,874	-	-	
TOTAL 0010					104	67	10,611	425	166	

204.0165 REMOVING GUARDRAIL

CATEGORY	STATION	TO	STATION	LOCATION	204.0165	REMARKS
					REMOVING GUARDRAIL LF	
0010	213+25	-	213+86	STH 131, RT	69	
TOTAL 0010					69	

638 MOVING SIGNS

CATEGORY	STATION	LOCATION	638.2102	638.4000	REMARKS
			MOVING SIGNS TYPE II EACH	MOVING SMALL SIGN SUPPORTS EACH	
0010	204+32	RT	1	1	ONE WAY, WOOD POST
0010	205+05	RT	1	1	STOP SIGN AHEAD, WOOD POST
0010	213+23	RT	1	1	STOP SIGN, WOOD POST
0010	213+25	RT	1	1	DO NOT ENTER, WOOD POST
TOTAL 0010			4	4	

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305 SHOULDER SUMMARY

CATEGORY	STATION	TO	STATION	LOCATION	305.0110	305.0500	624.0100	REMARKS
					BASE AGGREGATE DENSE 3/4-INCH TON	SHAPING SHOULDERS STA	WATER MGAL	
0010	206+49	-	208+13	LT	-	2	-	AT CTH C RESTORING SHOULDER AFTER REMOVING CURB AND GUTTER
0010	212+99	-	213+80	RT	17	-	0.3	
0010				UNDISTRIBUTED	3	-	0.1	
				TOTAL 0010	20	2	0.4	

460 HMA PAVEMENT

CATEGORY	STATION	TO	STATION	LOCATION	450.4000	455.0605	460.5424	REMARKS
					HMA COLD WEATHER PAVING TON	TACK COAT GAL	HMA PAVEMENT 4 LT 58-28 H TON	
0010	203+57	-	214+38	STH 131	536	335	536	
0010	215+90	-	229+33	STH 131	661	413	661	
				TOTAL 0010	1,197	748	1,197	
0020	209+79	-	212+18	RT	18	12	18	PARKING
0020	209+20	-	212+18	LT	23	14	23	PARKING
0020	216+79	-	221+19	LT	33	21	33	PARKING
0020	216+79	-	229+08	RT	92	58	92	PARKING
0020	221+61	-	224+98	LT	26	16	26	PARKING
0020	226+06	-	229+08	LT	23	15	23	PARKING
				TOTAL 0020	215	136	215	
				PROJECT TOTAL	1,412	884	1,412	

465 ASPHALTIC SURFACE PATCHING									
		211.0101.01 PREPARE FOUNDATION FOR ASPHALTIC PAVING (PROJECT) (01. 5780-03-63)		465.0105		465.0110		SPV.0180.01 SPECIAL (01. REMOVING DISTRESSED PAVEMENT MILLING)	
CATEGORY	STATION	TO	STATION	LOCATION	EACH	ASPHALTIC SURFACE TON	ASPHALTIC SURFACE PATCHING TON	SY	REMARKS
0010	203+57	-	229+33	PROJECT 5780-03-63	1	-	5	-	ASPHALTIC SURFACE PATCHING USED FOR MINOR REPAIRS DURING FOUNDATION PREPARATION ASPHALTIC SURFACE USED TO SPOT REPAIR AREAS OF PAVEMENT WEAKNESS WITHIN 16' OF CENTERLINE
0010	203+57	-	229+33	UNDISTRIBUTED	-	56	-	500	
0010	204+40	-	-	STH 131 AND PINE ST - SW QUADRANT	-	5	-	-	
0010	205+00	-	-	STH 131 AND PINE ST - SE QUADRANT	-	5	-	-	
0010	208+30	-	-	STH 131 AND CTH C - NW QUADRANT	-	5	-	-	
0010	10+40 CC	-	-	STH 131 AND CTH C - NE QUADRANT	-	3	-	-	
0010	208+80	-	-	STH 131 AND CTH C - SW QUADRANT	-	7	-	-	
0010	225+30	-	-	STH 131 AND 3RD ST - NW QUADRANT	-	2	-	-	
0010	225+90	-	-	STH 131 AND 3RD ST - NE QUADRANT	-	6	-	-	
0010	225+90	-	-	STH 131 AND 3RD ST - SE QUADRANT	-	3	-	-	
0010	229+25	-	-	STH 131 AND USH 61 - NW QUADRANT	-	2	-	-	
0010	229+25	-	-	STH 131 AND USH 61 - SW QUADRANT	-	1	-	-	
TOTAL 0010					1	95	5	500	

601 CURB AND GUTTER				
		601.0411 CONCRETE CURB & GUTTER 30-INCH TYPE D		601.0600 CONCRETE CURB PEDESTRIAN
CATEGORY	STATION	LOCATION	LF	LF
0010	204+40	STH 131 AND PINE ST - SW QUADRANT	41	-
0010	205+00	STH 131 AND PINE ST - SE QUADRANT	35	-
0010	208+30	STH 131 AND CTH C - NW QUADRANT	42	13
0010	10+40 CC	STH 131 AND CTH C - NE QUADRANT	29	19
0010	208+80	STH 131 AND CTH C - SW QUADRANT	51	26
0010	213+83	STH 131, RT	6	-
0010	225+30	STH 131 AND 3RD ST - NW QUADRANT	12	-
0010	225+90	STH 131 AND 3RD ST - NE QUADRANT	57	-
0010	225+90	STH 131 AND 3RD ST - SE QUADRANT	31	-
0010	229+25	STH 131 AND USH 61 - NW QUADRANT	14	16
0010	229+25	STH 131 AND USH 61 - SW QUADRANT	11	-
TOTAL 0010			329	74

602 CURB RAMPS						
CATEGORY	STATION	LOCATION	602.0405	602.0505	602.0605	REMARKS
			CONCRETE SIDEWALK 4-INCH SF	CURB RAMP DETECTABLE WARNING FIELD YELLOW SF	CURB RAMP DETECTABLE WARNING FIELD RADIAL YELLOW SF	
0010	204+40	STH 131 AND PINE ST - SW QUADRANT	181	-	21	
0010	205+00	STH 131 AND PINE ST - SE QUADRANT	143	-	21	
0010	208+30	STH 131 AND CTH C - NW QUADRANT	154	10	18	
0010	10+40 CC	STH 131 AND CTH C - NE QUADRANT	143	10	-	
0010	208+80	STH 131 AND CTH C - SW QUADRANT	256	10	-	
0010	225+30	STH 131 AND 3RD ST - NW QUADRANT	98	10	-	
0010	225+90	STH 131 AND 3RD ST - NE QUADRANT	180	20	-	
0010	225+90	STH 131 AND 3RD ST - SE QUADRANT	93	10	-	
0010	229+25	STH 131 AND USH 61 - NW QUADRANT	143	10	-	
0010	229+25	STH 131 AND USH 61 - SW QUADRANT	163	10	-	
TOTAL 0010			1,554	90	60	

611 UTILITY ADJUSTMENTS						
CATEGORY	STATION	LOCATION	611.8110	SPV.0060.01	SPV.0060.02	REMARKS
			ADJUSTING MANHOLE COVERS EACH	SPECIAL (01. ADJUSTING WATER VALVE BOXES) EACH	SPECIAL (02. ADJUSTING ENVIRONMENTAL MONITORING CAP) EACH	
0010	218+16	13' RT	1	-	-	STORM MANHOLE
0010	218+53	14' RT	1	-	-	STORM MANHOLE
0010	221+37	15' RT	1	-	-	STORM MANHOLE
0010	223+66	16' LT	-	-	1	
TOTAL 0010			3	0	1	
0030	203+96	7' RT	1	-	-	SANITARY MANHOLE
0030	204+66	4' LT	1	-	-	SANITARY MANHOLE
0030	206+50	3.2 LT	1	-	-	SANITARY MANHOLE
0030	217+28	16' RT	1	-	-	SANITARY MANHOLE
0030	217+64	10' RT	1	-	-	SANITARY MANHOLE
0030	221+63	12' RT	1	-	-	SANITARY MANHOLE
0030	221+84	14' LT	-	1	-	
0030	225+15	14' LT	-	1	-	
0030	225+34	11' LT	-	1	-	
0030	225+40	14' LT	-	1	-	
0030	225+43	11' LT	-	1	-	
0030	225+57	10' RT	1	-	-	SANITARY MANHOLE
0030	228+42	9' RT	1	-	-	SANITARY MANHOLE
TOTAL 0030			8	5	0	
PROJECT TOTAL			11	5	1	

614 BEAM GUARD SUMMARY

CATEGORY	STATION	TO	STATION	LOCATION	614.0010 BARRIER SYSTEM GRADING SHAPING FINISHING EACH	614.0305 STEEL PLATE BEAM GUARD CLASS A LF	614.0345 STEEL PLATE BEAM GUARD SHORT RADIUS LF	614.0390 STEEL PLATE BEAM GUARD SHORT RADIUS TERMINAL EACH	REMARKS
0010	213+09	-	213+51	STH 131, RT	1	-	50	1	
0010	213+51	-	213+86	STH 131, RT	-	38	-	-	
TOTAL 0010					1	38	50	1	

BARRIER SYSTEM GRADING SHAPING FINISHING INCIDENTAL ITEMS ESTIMATE

LOCATION	EXCAVATION COMMON *	BORROW *	SALVAGED TOPSOIL *	FERTILIZER TYPE B *	MIXTURE NO. 40 SEEDING *	EROSION MAT URBAN CLASS I TYPE A *
	CY	CY	SY	CWT	LB	SY
STA 212+98 - STA 213+86	31.00	0.00	181.00	0.06	8	181.00

* = ITEMS & QUANTITIES LISTED
FOR BID INFORMATION ONLY.

625 TURF ESTABLISHMENT

CATEGORY	STATION	LOCATION	625.0500 SALVAGED TOPSOIL SY	629.0210 FERTILIZER TYPE B CWT	630.0140 SEEDING MIXTURE NO. 40 LB	630.0500 SEED WATER MGAL	REMARKS
0010	204+40	STH 131 AND PINE ST - SW QUADRANT	17	0.01	0.8	0.3	
0010	205+00	STH 131 AND PINE ST - SE QUADRANT	16	0.01	0.7	0.3	
0010	208+30	STH 131 AND CTH C - NW QUADRANT	26	0.01	1.2	0.4	
0010	208+80	STH 131 AND CTH C - SW QUADRANT	3	0.01	0.2	0.1	
0010	225+30	STH 131 AND 3RD ST - NW QUADRANT	8	0.01	0.4	0.1	
0010	225+90	STH 131 AND 3RD ST - NE QUADRANT	30	0.01	1.4	0.5	
0010	225+90	STH 131 AND 3RD ST - SE QUADRANT	19	0.01	0.9	0.3	
0010	229+25	STH 131 AND USH 61 - NW QUADRANT	48	0.02	2.2	0.8	
0010	229+25	STH 131 AND USH 61 - SW QUADRANT	24	0.01	1.1	0.4	
0010		UNDISTRIBUTED	48	0.03	2.1	0.8	
TOTAL 0010			239	0.13	11	4	

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628 EROSION CONTROL														
CATEGORY	STATION	TO	STATION	LOCATION	628.1504	628.1520	628.1905	628.1910	628.2006	628.7005	628.7010	628.7015	628.7570	REMARKS
					SILT FENCE LF	SILT FENCE MAINTENANCE LF	MOBILIZATIONS EROSION CONTROL EACH	MOBILIZATIONS EMERGENCY EROSION CONTROL EACH	EROSION MAT URBAN CLASS I TYPE A SY	INLET PROTECTION TYPE A EACH	INLET PROTECTION TYPE B EACH	INLET PROTECTION TYPE C EACH	ROCK BAGS EACH	
0010	204+40	-	-	STH 131 AND PINE ST - SW QUADRANT	53	53	-	-	17	1	-	-	1	
0010	205+00	-	-	STH 131 AND PINE ST - SE QUADRANT	39	39	-	-	16	1	-	-	1	
0010	205+67	-	-	RT	-	-	-	-	-	-	-	1	-	
0010	208+30	-	-	STH 131 AND CTH C - NW QUADRANT	-	-	-	-	26	-	-	-	-	
0010	10+40 CC	-	-	STH 131 AND CTH C - NE QUADRANT	-	-	-	-	-	-	-	-	-	
0010	208+80	-	-	STH 131 AND CTH C - SW QUADRANT	-	-	-	-	3	-	-	-	-	
0010	209+00	-	-	LT	-	-	-	-	-	-	1	-	-	
0010	213+83	-	-	LT	-	-	-	-	-	-	-	1	-	
0010	218+15	-	-	LT	-	-	-	-	-	-	1	-	-	
0010	218+52	-	-	RT	-	-	-	-	-	-	-	1	-	
0010	221+72	-	-	LT	-	-	-	-	-	-	-	1	-	
0010	221+72	-	-	RT	-	-	-	-	-	-	-	1	-	
0010	225+30	-	-	STH 131 AND 3RD ST - NW QUADRANT	-	-	-	-	8	-	-	-	-	
0010	225+90	-	-	STH 131 AND 3RD ST - NE QUADRANT	-	-	-	-	30	-	-	-	1	
0010	225+90	-	-	STH 131 AND 3RD ST - SE QUADRANT	21	21	-	-	19	-	-	-	-	
0010	229+25	-	-	STH 131 AND USH 61 - NW QUADRANT	-	-	-	-	48	-	-	-	-	
0010	229+25	-	-	STH 131 AND USH 61 - SW QUADRANT	40	40	-	-	24	-	-	-	-	
0010	203+57	-	229+33	UNDISTRIBUTED	39	39	-	-	48	-	-	-	-	
0010	203+57	-	229+33	PROJECT 5780-03-63	-	-	2	2	-	-	-	-	-	
TOTAL 0010					192	192	2	2	239	2	2	5	3	

637 PERMANENT SIGNING													
CATEGORY	STATION	LOCATION	SIGN #	SIGN CODE	SIZE			SIGN HEIGHT FT	POST HEIGHT FT	634.0614	637.2210	DESCRIPTION	REMARKS
					IN	X	IN			POSTS WOOD 4X6-INCH X 14-FT EACH	SIGNS TYPE II REFLECTIVE H SF		
0010	229+93	LT	1	W11-2	30	X	30	2.5	13.75	1	6.25	WARNING SIGN	PEDESTRIAN CROSSING
0010				W16-9P	24	X	12	1.0	-	-	2.00	WARNING SIGN PLAQUE	AHEAD
TOTAL 0010										1	8.25		

643.1050 PCMS				
CATEGORY	STATION	LOCATION	643.1050	REMARKS
			TRAFFIC CONTROL SIGNS PCMS DAY	
0010	203+57	STH 131	7	BEGIN OF PROJECT 5780-03-63
0010	229+33	STH 131	7	END OF PROJECT 5780-03-63
TOTAL 0010			14	

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643 TRAFFIC CONTROL

					643.0300			643.0900			643.3165	643.3180	643.3305	643.3350	643.5000	646.9000	REMARKS
					TRAFFIC CONTROL DRUMS			TRAFFIC CONTROL SIGNS			TEMPORARY MARKING LINE PAINT 6-INCH *	TEMPORARY MARKING LINE REMOVABLE TAPE 6-INCH	TEMPORARY MARKING CROSSWALK PAINT 6-INCH	TEMPORARY MARKING CROSSWALK REMOVABLE TAPE 6-INCH	TRAFFIC CONTROL	MARKING REMOVAL LINE 4-INCH	
CATEGORY	STATION	TO	STATION	LOCATION	EACH	DAYS OF WORK	DAY	EACH	DAYS OF WORK	DAY	LF	LF	LF	LF	EACH	LF	
0010	202+15	-	207+22	PINE STREET	38	6	228	4	6	24	-	1,016	78	-	-	1,014	TEMPORARY PEDESTRIAN DETOUR
0010	227+92	-	209+60	CTH C (STAGE 1)	34	6	204	7	6	42	-	-	66	66	-	-	TEMPORARY PEDESTRIAN DETOUR
0010	207+05	-	209+57	CTH C (STAGE 2)	30	6	180	3	6	18	-	-	66	-	-	-	TEMPORARY PEDESTRIAN DETOUR
0010	223+13	-	228+15	3RD ST (STAGE 1)	33	6	198	4	6	24	1,004	-	82	-	-	1,004	TEMPORARY PEDESTRIAN DETOUR
0010	223+77	-	228+15	3RD ST (STAGE 2)	24	6	144	4	6	24	876	-	52	-	-	876	TEMPORARY PEDESTRIAN DETOUR
0010	227+14	-	230+38	USH 61	34	6	204	5	6	30	-	641	-	-	-	648	TEMPORARY PEDESTRIAN DETOUR
0010	203+57	-	208+18	CL	-	-	-	-	-	-	922	-	-	-	-	-	DOUBLE YELLOW
0010	208+79	-	217+90	CL	-	-	-	-	-	-	1,822	-	-	-	-	-	DOUBLE YELLOW
0010	217+90	-	221+51	CL	-	-	-	-	-	-	390	-	-	-	-	-	SOLID - SHORT-SKIP
0010	221+51	-	223+84	CL	-	-	-	-	-	-	19	-	-	-	-	-	SHORT-SKIP
0010	223+84	-	227+43	CL	-	-	-	-	-	-	388	-	-	-	-	-	SOLID - SHORT-SKIP
0010	227+43	-	229+33	CL	-	-	-	-	-	-	380	-	-	-	-	-	DOUBLE YELLOW
0010	203+57	-	229+33	PROJECT 5780-03-63	-	-	-	18	50	900	-	-	-	-	1	-	ADVANCE WARNING SIGNING
0010	203+57	-	229+33	PROJECT 5780-03-63	-	-	-	6	4	24	-	-	-	-	-	-	FLAGGING SIGNING
0010	203+57	-	229+33	PROJECT 5780-03-63	-	-	-	2	1	2	-	-	-	-	-	-	ROLLING CLOSURE SIGNING
TOTAL 0010					1,158			1,088			5,801	1,657	344	66	1	3,542	

* USED ALONG CENTERLINE ON MILLED SURFACE OR FOR TEMPORARY PEDESTRIAN DETOURS

644 TEMPORARY PEDESTRIAN

					644.1430	644.1601			644.1605	644.1810	REMARKS
					TEMPORARY PEDESTRIAN SURFACE PLATE	TEMPORARY PEDESTRIAN CURB RAMP			TEMPORARY PEDESTRIAN DETECTABLE WARNING FIELD	TEMPORARY PEDESTRIAN BARRICADE	
CATEGORY	STATION	TO	STATION	LOCATION	SF	EACH	DAYS OF WORK	DAY	SF	LF	
0010	202+24	-	207+22	PINE STREET	80	2	6	12	16	220	
0010	227+92	-	209+60	CTH C (STAGE 1)	-	4	6	24	-	215	
0010	207+05	-	209+57	CTH C (STAGE 2)	-	1	6	6	10	145	
0010	223+13	-	228+15	3RD ST (STAGE 1)	160	2	6	12	20	230	
0010	223+77	-	228+15	3RD ST (STAGE 2)	160	2	6	12	10	175	
0010	227+14	-	230+38	USH 61	150	2	6	12	-	160	
TOTAL 0010					550	78			56	1,145	

646 PERMANENT PAVEMENT MARKING								
CATEGORY	STATION	TO	STATION	LOCATION	646.4720	646.6466	646.7520	REMARKS
					MARKING LINE SAME DAY EPOXY 6-INCH LF	COLD WEATHER MARKING EPOXY 6-INCH LF	MARKING CROSSWALK EPOXY BLOCK STYLE 24-INCH LF	
0010	202+15	-	208+18	CENTERLINE	1,206	1,206	-	DOUBLE YELLOW
0010	208+79	-	217+90	CENTERLINE	1,822	1,822	-	DOUBLE YELLOW
0010	217+90	-	221+51	CENTERLINE	452	452	-	SOLID-SKIP
0010	221+51	-	223+84	CENTERLINE	59	59	-	SKIP
0010	223+84	-	227+43	CENTERLINE	449	449	-	SOLID-SKIP
0010	227+43	-	230+38	CENTERLINE	590	590	-	DOUBLE YELLOW
0010	229+23	-	229+29	LT/RT	-	-	54	
TOTAL 0010					4,578	4,578	54	

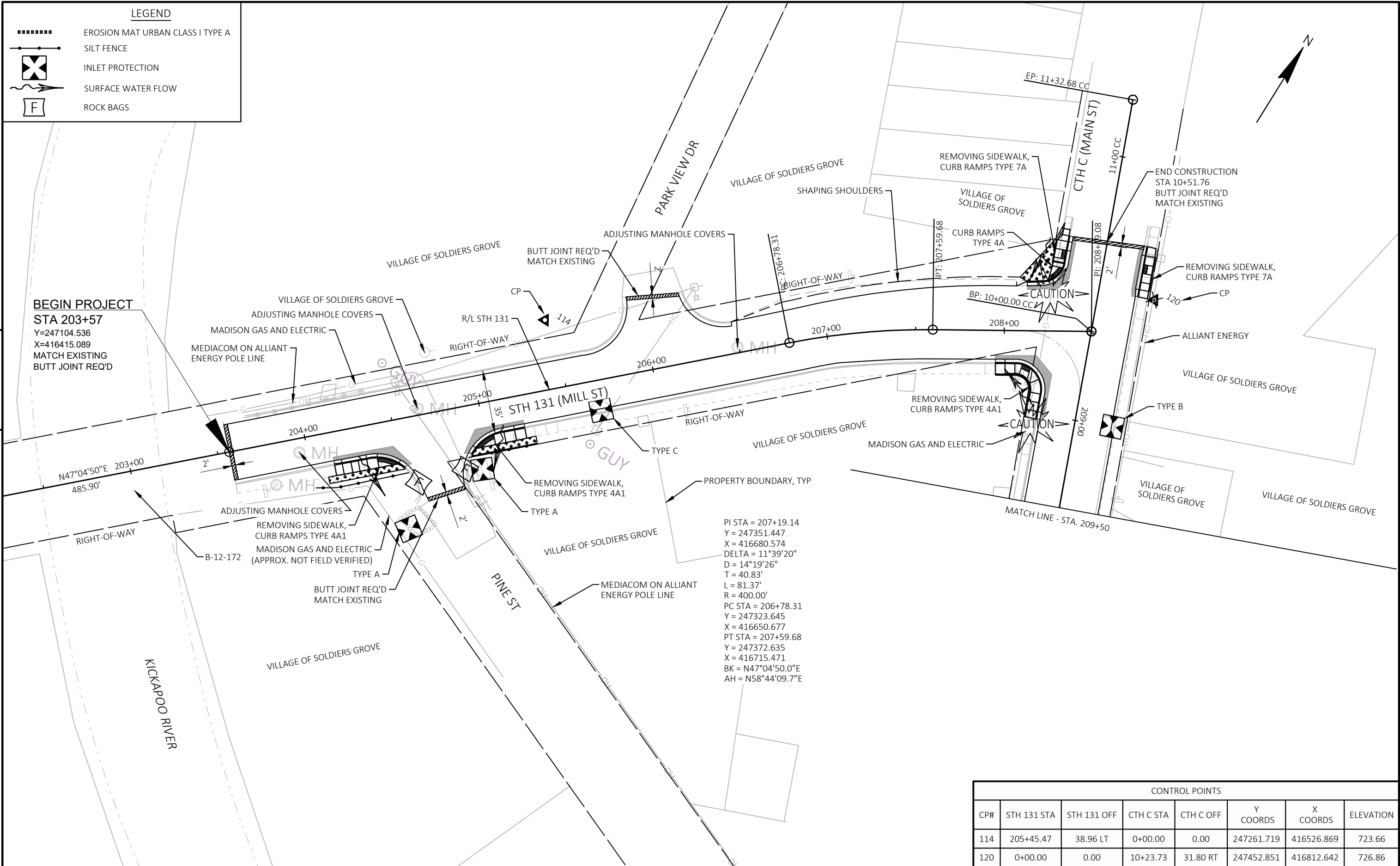
650 CONSTRUCTION STAKING								
CATEGORY	STATION	TO	STATION	LOCATION	650.8000	650.9000	650.9911.01	REMARKS
					CONSTRUCTION STAKING RESURFACING REFERENCE LF	CONSTRUCTION STAKING CURB RAMPS EACH	CONSTRUCTION STAKING SUPPLEMENTAL CONTROL (PROJECT) (01. 5780-03-63) EACH	
0010	204+40	-	-	STH 131 AND PINE ST - SW QUADRANT	-	1	-	
0010	205+00	-	-	STH 131 AND PINE ST - SE QUADRANT	-	1	-	
0010	208+30	-	-	STH 131 AND CTH C - NW QUADRANT	-	2	-	
0010	10+40 CC	-	-	STH 131 AND CTH C - NE QUADRANT	-	1	-	
0010	208+80	-	-	STH 131 AND CTH C - SW QUADRANT	-	1	-	
0010	225+30	-	-	STH 131 AND 3RD ST - NW QUADRANT	-	1	-	
0010	225+90	-	-	STH 131 AND 3RD ST - NE QUADRANT	-	2	-	
0010	225+90	-	-	STH 131 AND 3RD ST - SE QUADRANT	-	1	-	
0010	229+25	-	-	STH 131 AND USH 61 - NW QUADRANT	-	1	-	
0010	229+25	-	-	STH 131 AND USH 61 - SW QUADRANT	-	1	-	
0010	203+57	-	214+38	STH 131	1,081	-	-	
0010	215+90	-	229+33	STH 131	1,343	-	-	
0010	203+57	-	229+33	PROJECT 5780-03-63	-	-	1	
TOTAL 0010					2,424	12	1	

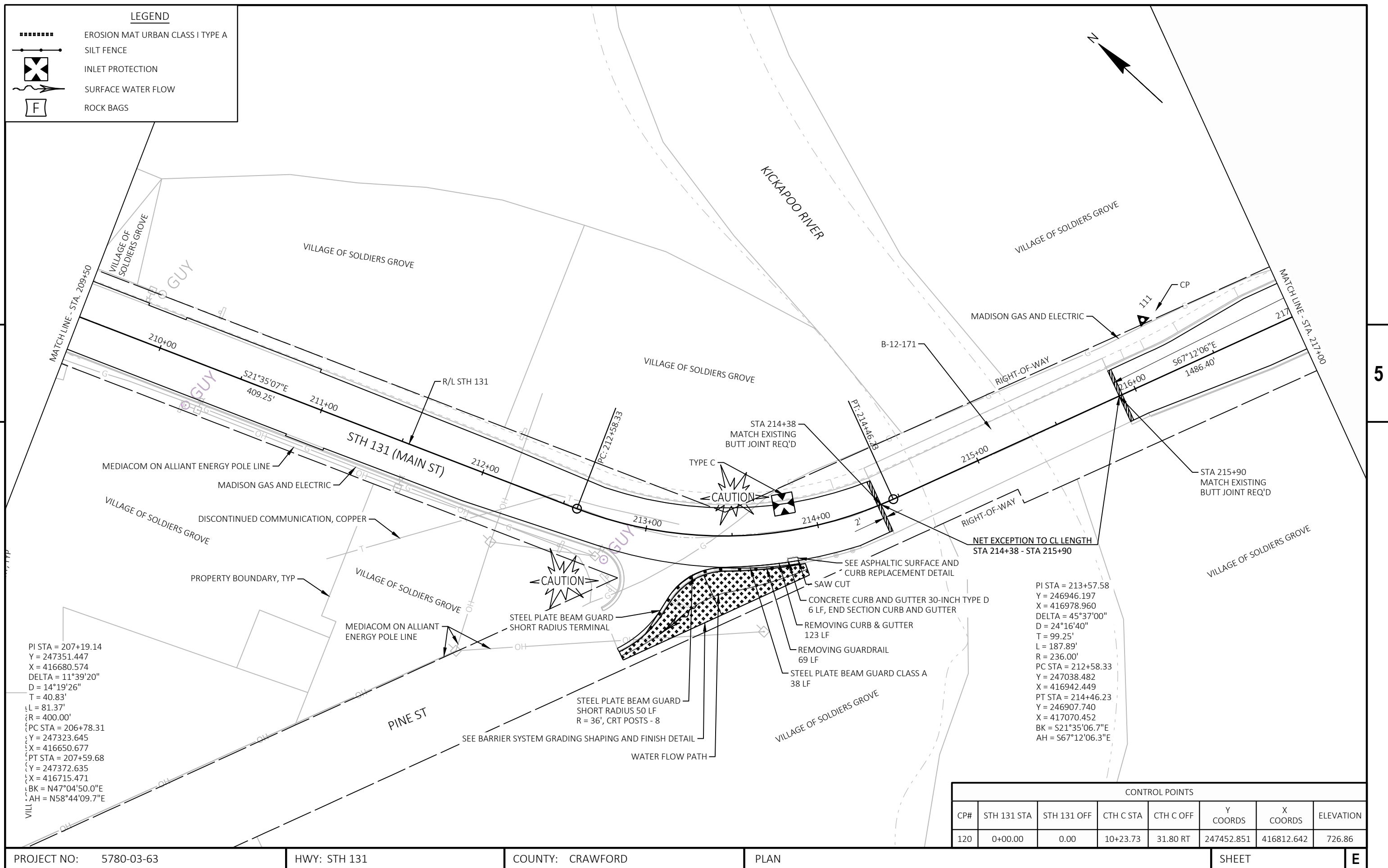
690.0150 SAWING ASPHALT

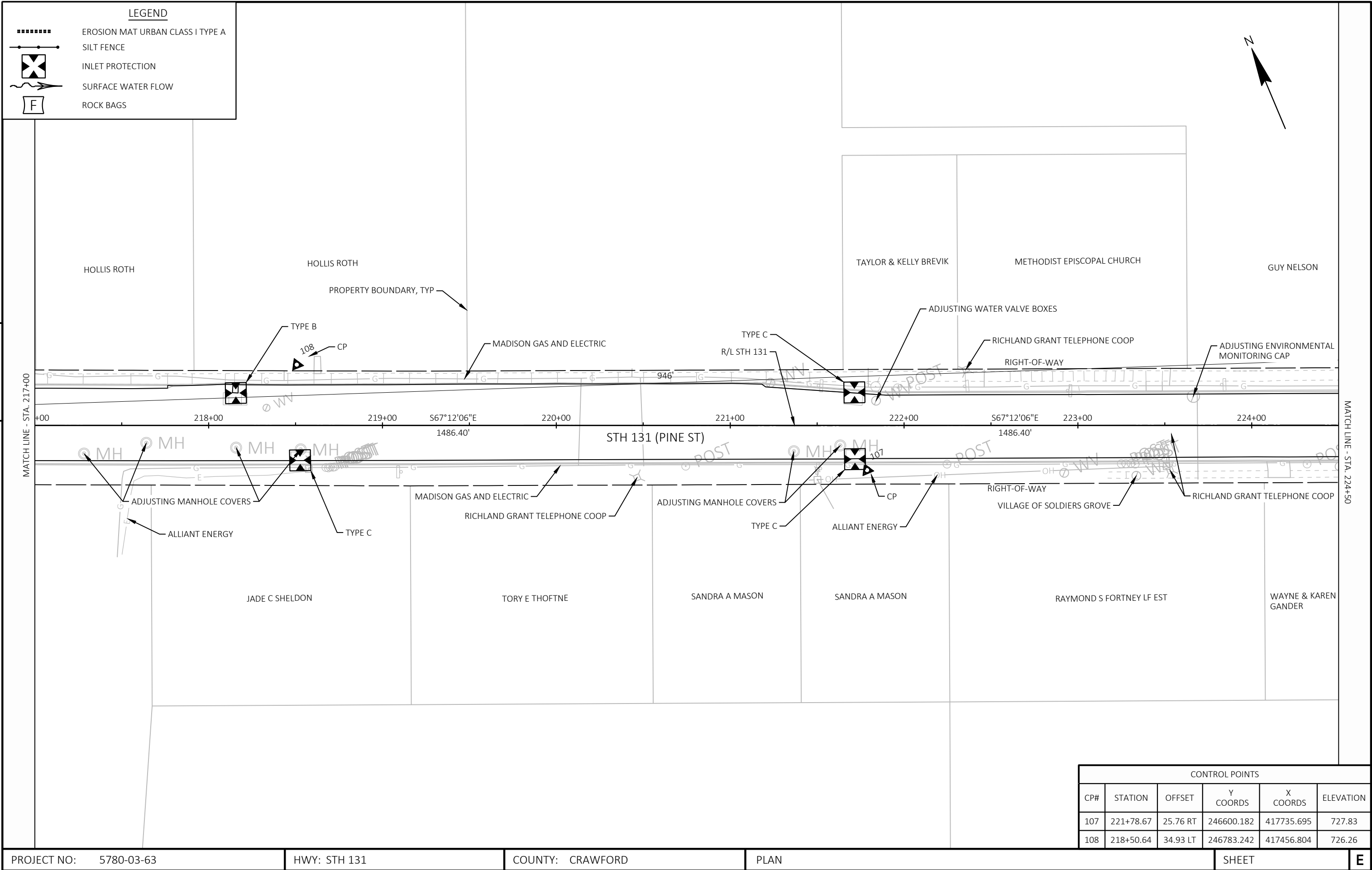
690.0150 SAWING ASPHALT						REMARKS
CATEGORY	STATION	TO	STATION	LOCATION	LF	
0010	20414	-	20456	STH 131 RT	53.000000	
0010	20487	-	20523	STH 131 RT	47.000000	
0010	20795	-	20903	STH 131 RT	67.000000	
0010	20813	-	20840	STH 131 LT	53.000000	
0010	10+23 CC	-	10+53 CC	CTH C 19' RT	33	
0010	10+24 CC	-	10+52 CC	CTH C 29' RT	31	
0010	22531	-	22538	STH 131 LT	20.000000	
0010	22565	-	22615	STH 131 LT	66.000000	
0010	22583	-	22614	STH 131 RT	35.000000	
0010	22917	-	22932	STH 131 LT	19.000000	
0010	22921	-	22932	STH 131 RT	15	
TOTAL 0010					439	

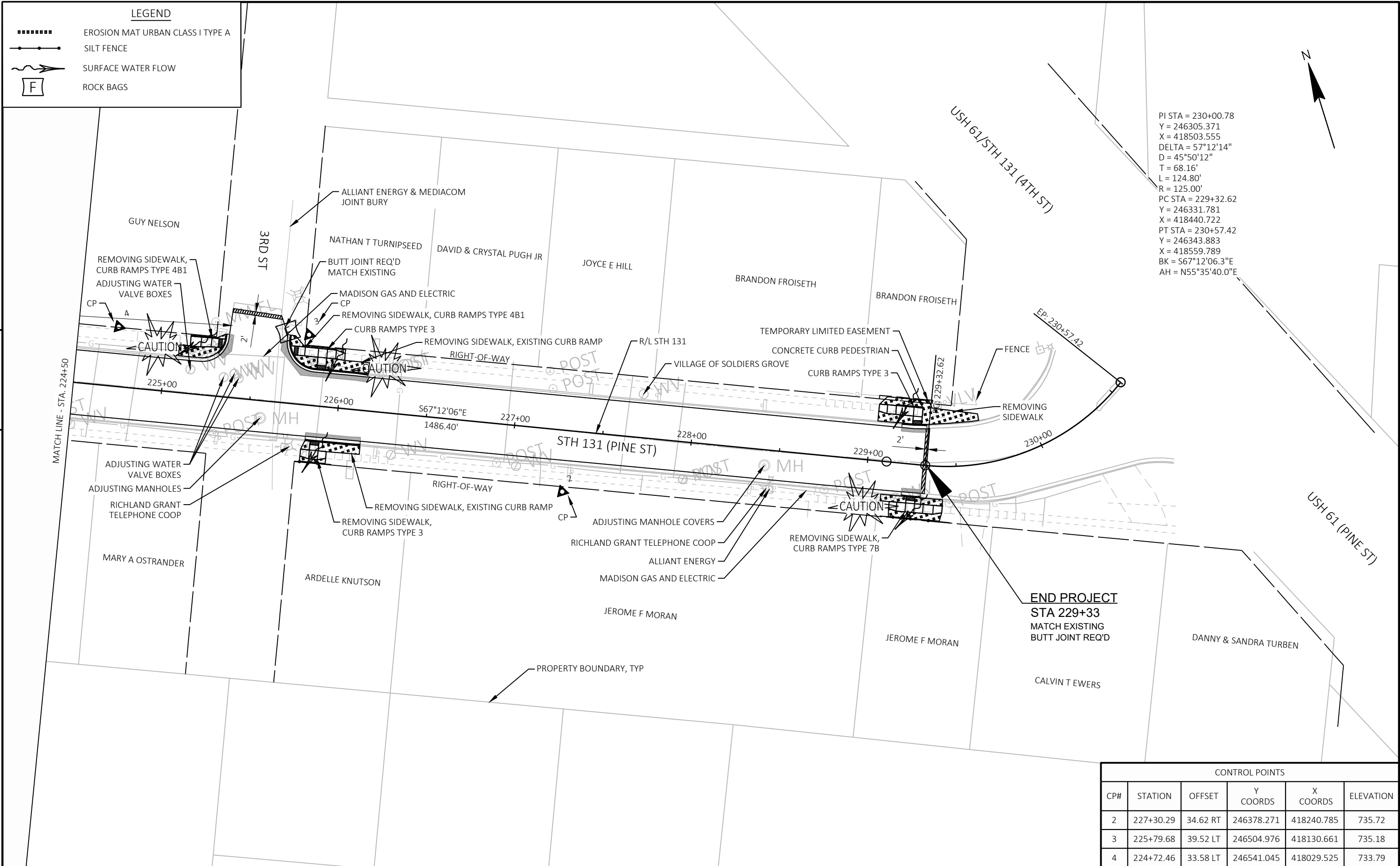
690.0250 SAWING CONCRETE

690.0250 SAWING CONCRETE						REMARKS
CATEGORY	STATION	TO	STATION	LOCATION	LF	
0010	204+14	-	204+14	STH 131 RT	9	
0010	204+52	-	204+54	STH 131 RT	3	
0010	204+89	-	204+91	STH 131 RT	3	
0010	205+23	-	205+23	STH 131 RT	8	
0010	10+24 CC	-	10+24 CC	CTH C RT	7	
0010	10+51 CC	-	10+51 CC	CTH C LT	2	
0010	10+52 CC	-	10+53 CC	CTH C RT	7	
0010	10+54 CC	-	10+55 CC	CTH C LT	4	
0010	207+95	-	207+95	STH 131 RT	8	
0010	209+03	-	209+03	STH 131 RT	8	
0010	213+85	-	213+86	STH 131 RT	2	
0010	225+14	-	225+14	STH 131 LT	6	
0010	225+29	-	225+31	STH 131 LT	2	
0010	225+81	-	225+81	STH 131 RT	4	
0010	225+83	-	225+83	STH 131 RT	2	
0010	225+96	-	225+96	STH 131 RT	4	
0010	226+01	-	226+01	STH 131 LT	4	
0010	226+14	-	226+14	STH 131 RT	3	
0010	226+15	-	226+15	STH 131 LT	2	
0010	229+04	-	229+04	STH 131 LT	4	
0010	229+14	-	229+14	STH 131 RT	4	
0010	229+17	-	229+17	STH 131 LT	3	
0010	229+21	-	229+21	STH 131 RT	3	
0010	229+31	-	229+32	STH 131 LT	2	
0010	229+32	-	229+32	STH 131 RT	3	
0010	229+42	-	229+43	STH 131 RT	4	
TOTAL 0010					111	









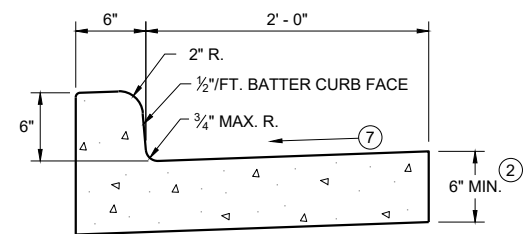
PI STA = 230+00.78
Y = 246305.371
X = 418503.555
DELTA = 57°12'14"
D = 45°50'12"
T = 68.16'
L = 124.80'
R = 125.00'
PC STA = 229+32.62
Y = 246331.781
X = 418440.722
PT STA = 230+57.42
Y = 246343.883
X = 418559.789
BK = S67°12'06.3"E
AH = N55°35'40.0"E

END PROJECT
STA 229+33
MATCH EXISTING
BUTT JOINT REQ'D

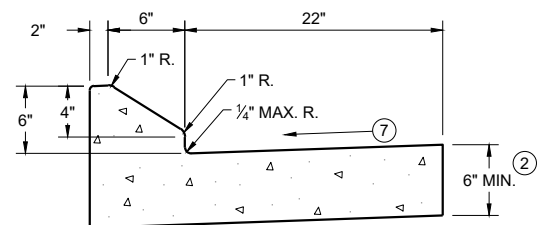
CONTROL POINTS					
CP#	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
2	227+30.29	34.62 RT	246378.271	418240.785	735.72
3	225+79.68	39.52 LT	246504.976	418130.661	735.18
4	224+72.46	33.58 LT	246541.045	418029.525	733.79

Standard Detail Drawing List

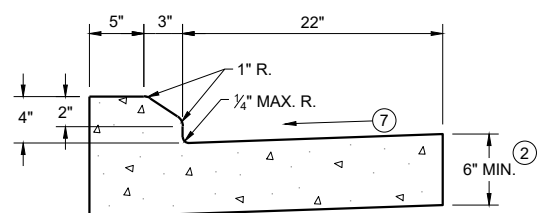
08D01-23A	CONCRETE CURB & GUTTER
08D01-23B	CONCRETE CURB, TIES AND CURB AND GUTTER APPLI CATIONS
08D05-21A	CURB RAMPS TYPES 1 AND 1-A
08D05-21B	CURB RAMPS TYPES 2 AND 3
08D05-21C	CURB RAMPS TYPES 4A AND 4A1
08D05-21D	CURB RAMPS TYPE 4B AND 4B1
08D05-21E	CURB RAMPS TYPES 5, 6, 7A, 7B & 8
08D05-21F	CURB RAMPS RADIAL DETECTABLE WARNING FIELD APPLICATIONS
08D05-21G	CURB RAMPS RECTANGULAR AND RADIAL DETECTABLE WARNING PLATES
08E09-06	SILT FENCE
08E10-02	INLET PROTECTION TYPE A, B, C AND D
11B02-02	CONCRETE MEDI AN NOSE
13C19-03	HMA LONGITUDINAL JOINTS
14B15-11A	STEEL PLATE BEAM GUARD, CLASS "A" INSTALLATION & ELEMENTS
14B15-11B	STEEL PLATE BEAM GUARD, CLASS "A" INSTALLATION & ELEMENTS
14B15-11C	STEEL PLATE BEAM GUARD, CLASS "A", INSTALLATION & ELEMENTS
14B18-06A	STEEL PLATE BEAM GUARD, CLASS "A" (AT BRIDGES, OBSTACLES AND S I DERROADS/DRI VEWAYS)
14B27-01A	STEEL PLATE BEAM GUARD SHORT RADIUS TERMINAL
14B27-01B	STEEL PLATE BEAM GUARD SHORT RADIUS TERMINAL
14B27-01C	STEEL PLATE BEAM GUARD SHORT RADIUS TERMINAL
15C02-09A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-09F	ADVANCED WIDTH RESTRI CTION SIGNING
15C03-05	BARRICADES AND SIGNS FOR SIDEROAD CLOSURES
15C05-05	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M. P. H. OR LESS
15C08-23A	PERMANENT LONGI TUDINAL PAVEMENT MARKINGS
15C08-23B	TEMPORARY LONGI TUDINAL PAVEMENT MARKING
15C11-10A	CHANNELIZING DEVICES FLEXIBLE TUBULAR MARKER POST
15C11-10B	CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS
15C12-09A	TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION
15C12-09B	TRAFFIC CONTROL, LANE CLOSURE WITH AUTOMATED FLAGGER ASSI STANCE DEVICE
15C19-09A	MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY
15C33-05	STOP LINE AND CROSSWALK PAVEMENT MARKING
15D06-06	TRAFFIC CONTROL, TWO LANE TWO WAY OPERATION
15D28-04	TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDI VI DED ROADWAY
15D30-09A	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09B	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09C	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATI ON
15D39-02	TRAFFIC CONTROL, DROP-OFF SIGNING
15D44-02	TRAFFIC CONTROL, SIGNING ON ROADWAYS WITH MILLED SURFACES
15D48-01	TRAFFIC CONTROL, LANE SHIFT IN FLAGGING OPERATION
15D51-01	TRAFFIC CONTROL, MOBILE OPERATIONS ON AN UNDI VI DED ROADWAY



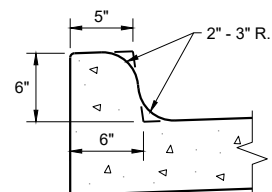
TYPES A^① & D



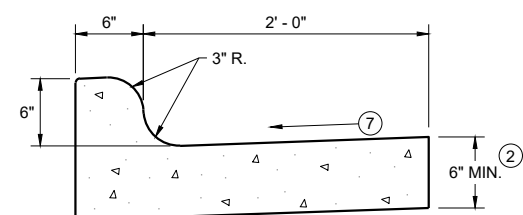
6" SLOPED CURB TYPES G ^① & J



4" SLOPED CURB TYPES G^① & J

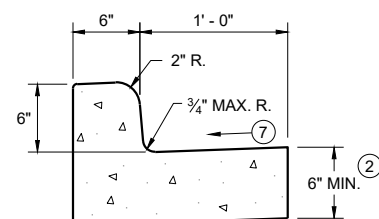


TYPES K^① & L
(OPTIONAL CURB SHAPE)



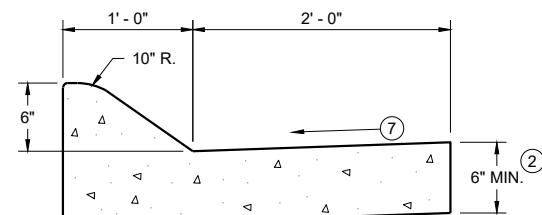
TYPES K¹ & L

CONCRETE CURB AND GUTTER 30"

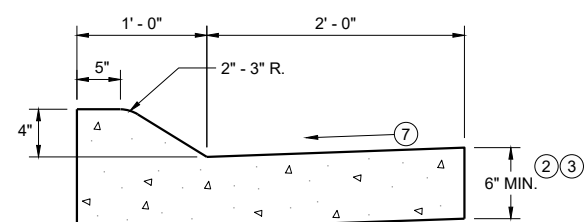


TYPES A^① & D

CONCRETE CURB AND GUTTER 18"

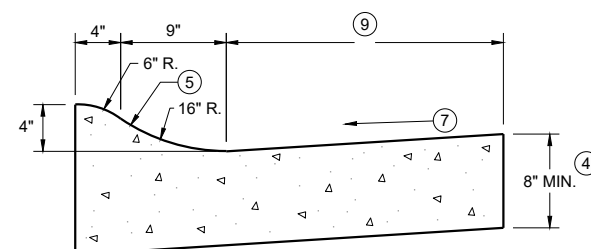


6" SLOPED CURB TYPES A^① & D



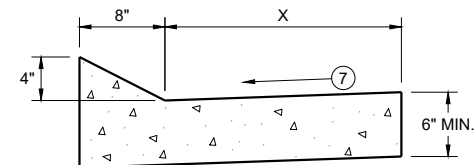
4" SLOPED CURB TYPES A ^① & D

CONCRETE CURB AND GUTTER 36"



4" SLOPED CURB TYPES R^① & T

TBT & TBTT	X
30"	22"
36"	28"

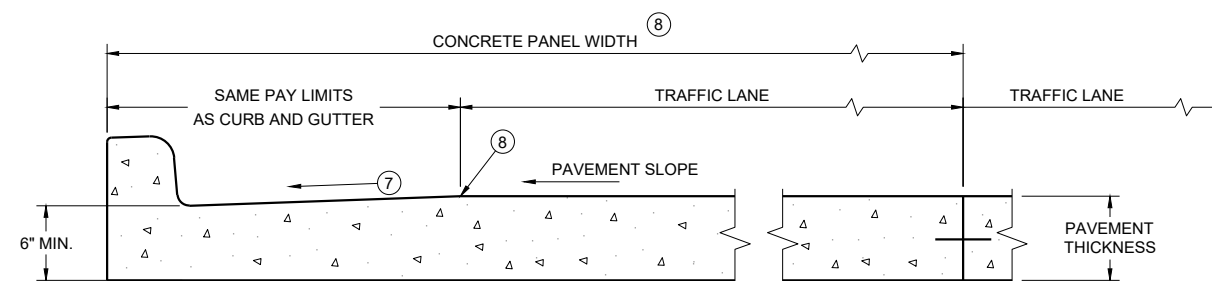


TYPES TBT & TBTT ^①

CONCRETE CURB AND GUTTER

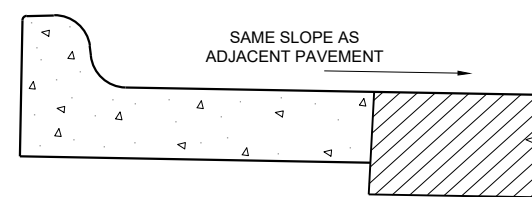
PAVEMENT THICKNESS AND MAXIMUM CONCRETE PANEL WIDTH TABLE

PAVEMENT THICKNESS	MAXIMUM PANEL WIDTH
LESS THAN 10"	12'
10" & ABOVE	15'



PARTIAL SECTION OF PAVEMENT WITH INTEGRAL CURB AND GUTTER *

* BIKE LANE IS NOT SHOWN



REVERSE SLOPE GUTTER

(TYPICAL FOR ALL CURB & GUTTER TYPES)

CONCRETE CURB AND GUTTER

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

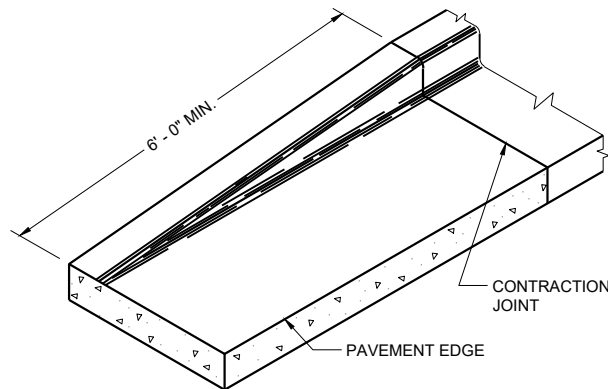
DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

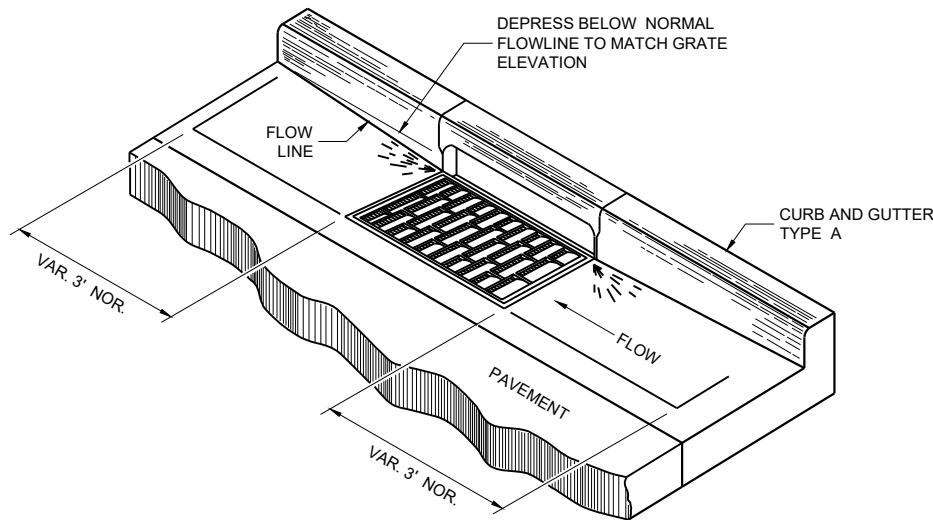
INTEGRAL CURB AND GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB AND GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ③ USE 8" MINIMUM GUTTER THICKNESS WHEN USED WITH AN ADJACENT CONCRETE TRUCK APRON PLACED BEHIND BACK OF CURB.
- ④ THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑤ UNLESS OTHERWISE NOTED, FOR STAKING PURPOSES THE FACE OF CURB IS 6" FROM THE BACK OF CURB.
- ⑥ WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.
- ⑦ USE 4% GUTTER CROSS SLOPE UNLESS OTHERWISE NOTED IN THE PLANS.
- ⑧ INCLUDE LONGITUDINAL JOINT AND TIE BARS ALONG LANE EDGE WHEN CONCRETE PANEL WIDTH EXCEEDS THE MAXIMUM WIDTH PER TABLE BELOW. LONGITUDINAL JOINT(S) ARE NOT ALLOWED WITHIN TRAFFIC LANES AND BIKE LANES. LONGITUDINAL JOINT MAY BE SAWED.
- ⑨ CONCRETE CURB AND GUTTER 4-INCH SLOPED 30-INCH TYPE "R" AND "T" = 17 INCHES
CONCRETE CURB AND GUTTER 4-INCH SLOPED 36-INCH TYPE "R" AND "T" = 23 INCHES

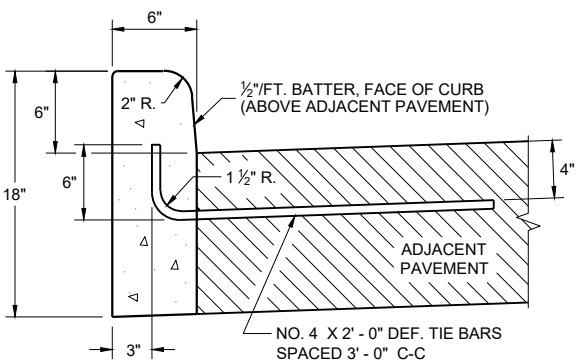


END SECTION CURB AND GUTTER

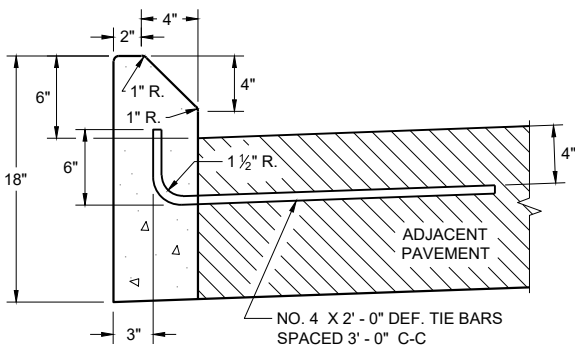


DETAIL OF CURB AND GUTTER AT INLETS

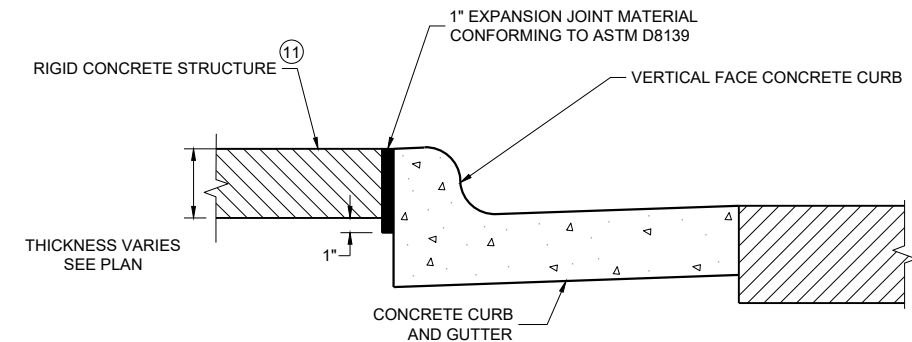
(TYPICAL H INLET COVER SHOWN)



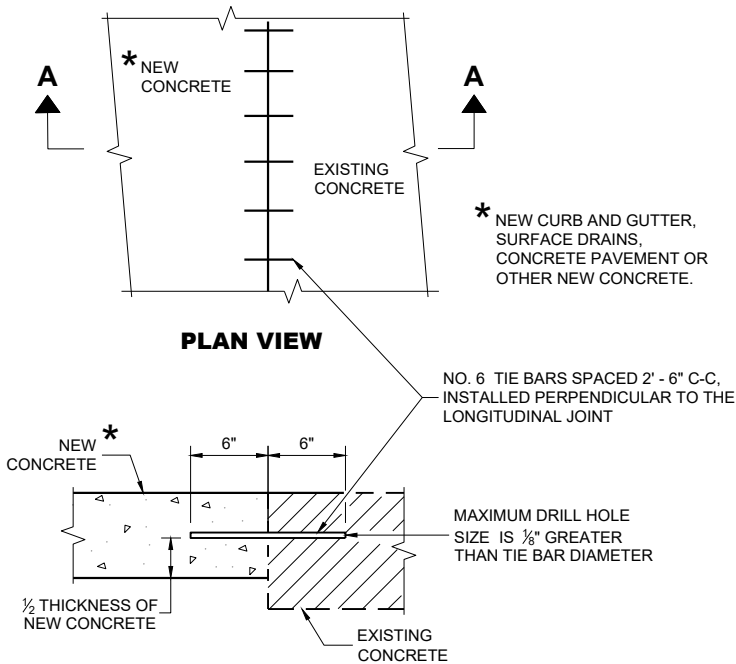
TYPES A^① & D



TYPES G^① & J
CONCRETE CURB



EXPANSION JOINT DETAIL FOR VERTICAL CURB ABUTTING A RIGID STRUCTURE^⑪



SECTION A - A
TIE BARS DRILLED INTO EXISTING PAVEMENT

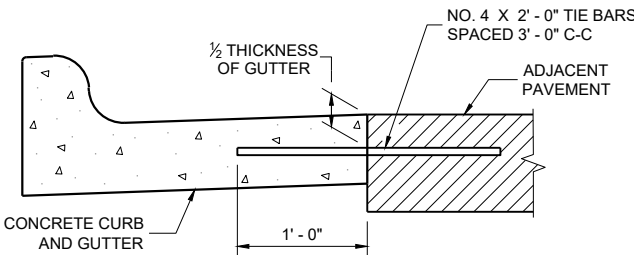
GENERAL NOTES

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

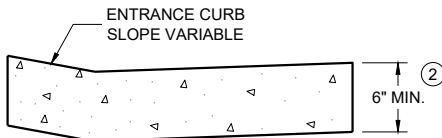
PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑩ REFER TO SDD 08D18 AND 08D19 FOR ADDITIONAL DRIVEWAY ENTRANCE CURB DETAILS.
- ⑪ PLACE 1" THICK EXPANSION JOINT MATERIAL BETWEEN VERTICAL FACE CURB TYPES EXTENDING FROM THE TOP OF CURB TO 1 INCH BELOW THE ADJOINING CONCRETE SURFACE. RIGID CONCRETE STRUCTURES INCLUDE RAISED CONCRETE MEDIANS, CONCRETE SAFETY ISLANDS, SPLITTER ISLANDS, OR LOCATIONS IDENTIFIED ON THE PLANS.



TYPICAL TIE BAR LOCATION^①

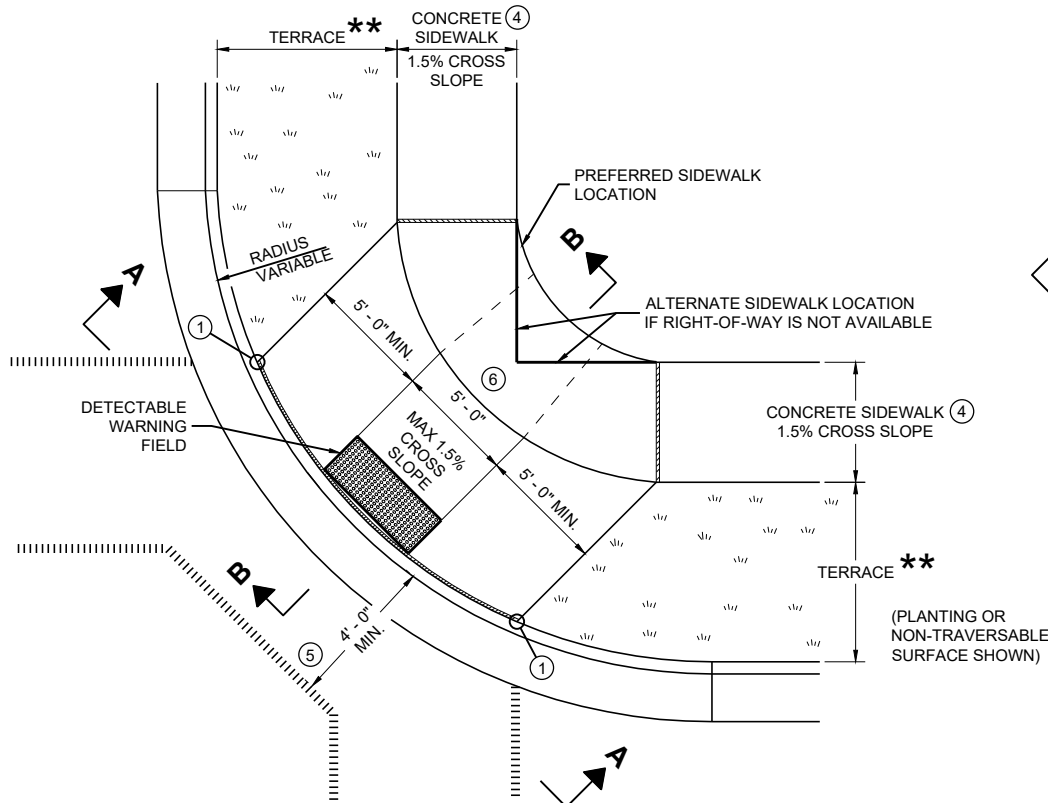


DRIVEWAY ENTRANCE CURB^⑩
(WHEN DIRECTED BY THE ENGINEER)

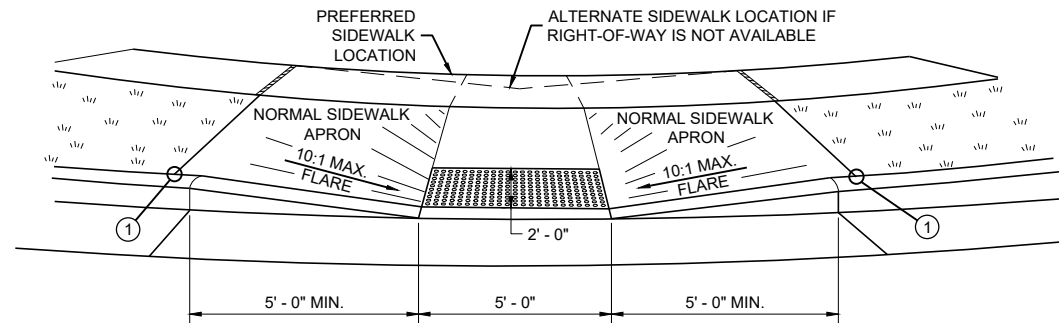
CONCRETE CURB, TIES
AND CURB AND GUTTER
APPLICATIONS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
FHWA UNIT SUPERVISOR

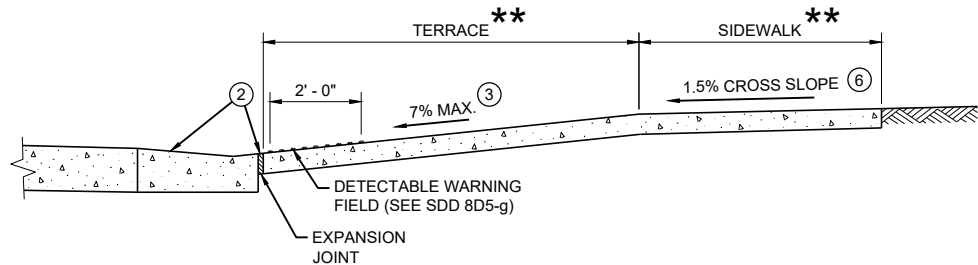


PLAN VIEW
CURB RAMP TYPE 1
(CENTER OF CORNER RADIUS)

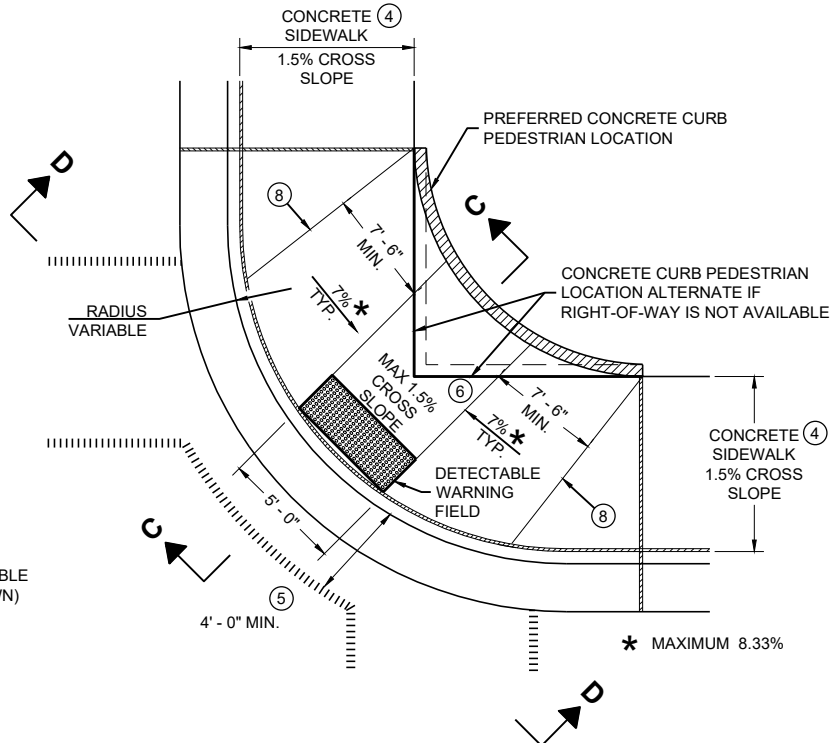


VIEW A - A FOR TYPE 1

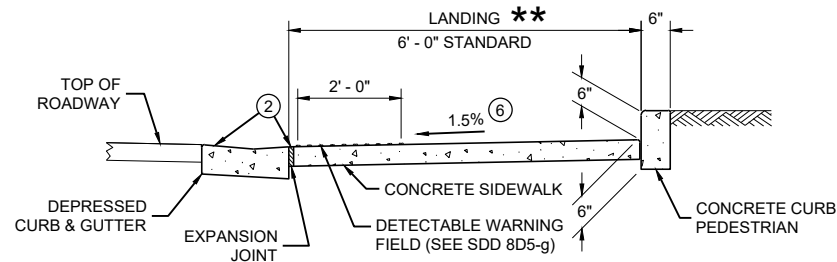
** WIDTH SHOWN ELSEWHERE
IN THE PLANS



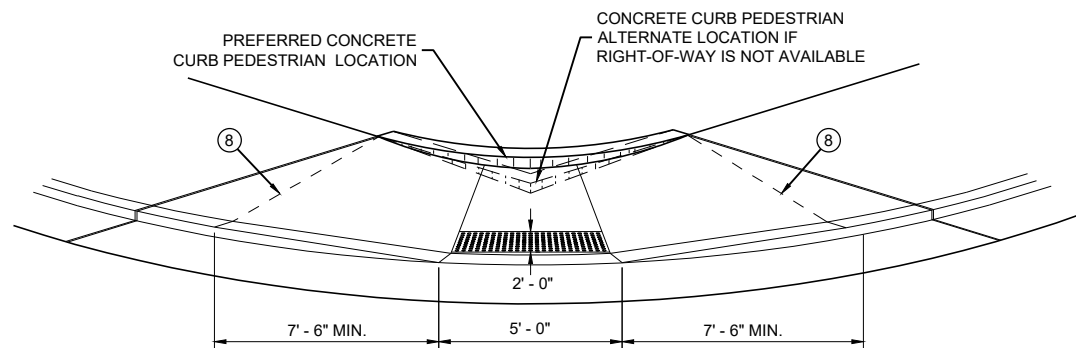
SECTION B - B FOR TYPE 1



PLAN VIEW
CURB RAMP TYPE 1 - A
(NO TERRACE)



SECTION C - C FOR TYPE 1 - A



VIEW D - D FOR TYPE 1 - A

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

WHEN NECESSARY, THE SIDEWALK ELEVATION MAY BE LOWERED TO MEET THE HIGH POINT ON THE RAMP.

TYPE 1 CURB RAMPS SHALL HAVE A NORMAL SIDEWALK APRON AND CURB ON BOTH SIDES OF RAMP.

DETECTABLE WARNING FIELD SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS "CURB RAMP DETECTABLE WARNING FIELD". THE CONCRETE PEDESTRIAN CURB, IF NEEDED, SHALL BE MEASURED AND PAID BY THE LINEAR FOOT AS "CONCRETE CURB PEDESTRIAN". CONCRETE SIDEWALK IN THE CURB RAMP AREA SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS CONCRETE SIDEWALK, INCLUDING THE AREA UNDER THE DETECTABLE WARNING FIELD.

SELECT CURB RAMP DETECTABLE WARNING FIELD MATERIALS AND DEVICES FROM THE DEPARTMENT'S APPROVED MATERIALS LIST. THE COLOR OF THE DETECTABLE WARNING FIELD IS SPECIFIED ELSEWHERE AND IS INCIDENTAL TO THE BID ITEM OF "CURB RAMP DETECTABLE WARNING FIELD"

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

SURFACE TEXTURE OF THE RAMP SHALL BE OBTAINED BY COARSE BROOMING TRANSVERSE TO THE SLOPE OF THE RAMP.

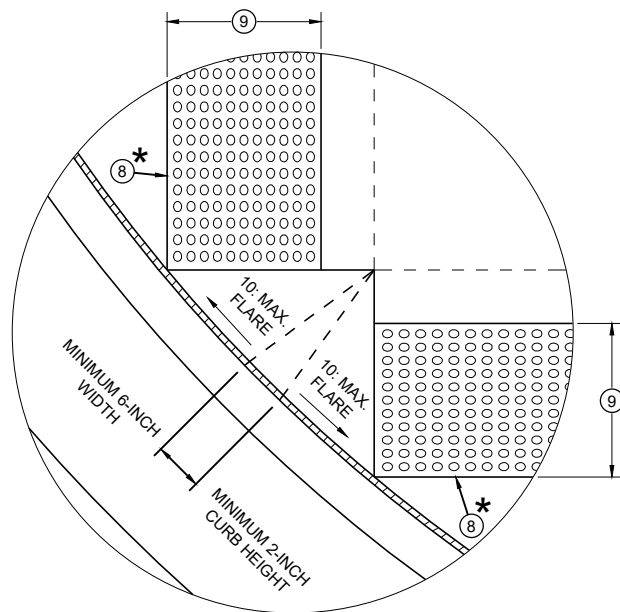
- ① THIS POINT IS AN EXTENSION OF OUTSIDE EDGE OF APPROACHING SIDEWALK WHERE IT MEETS THE BACK OF CONCRETE CURB. POINT LOCATION MAY BE ADJUSTED TO ALIGN WITH BEGINNING OF FULL-HEIGHT CURB IF THIS DISTANCE IS SHORT.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ MAXIMUM 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑤ PROVIDE A LEVEL LANDING IN THE STREET AND GUTTER AREA (2% MAXIMUM SLOPE IN ANY DIRECTION). WHEN THE GUTTER SLOPE EXCEEDS 2%, CONSTRUCT THE LEVEL LANDING IN THE STREET AREA. 4 FOOT WIDTH IS MEASURED FROM THE FLANGE LINE
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

LEGEND

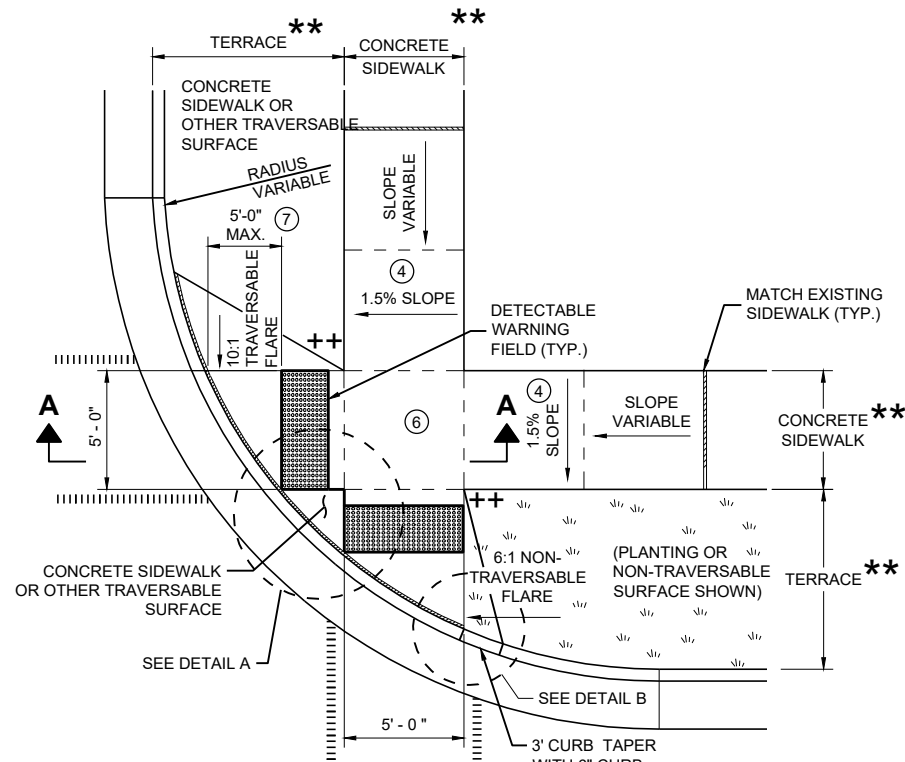
- ===== 1/2" EXPANSION JOINT SIDEWALK
- - - - - CONTRACTION JOINT FIELD LOCATED
- ||||| PAVEMENT MARKING CROSSWALK (WHITE)

CURB RAMPS
TYPE 1 AND 1-A

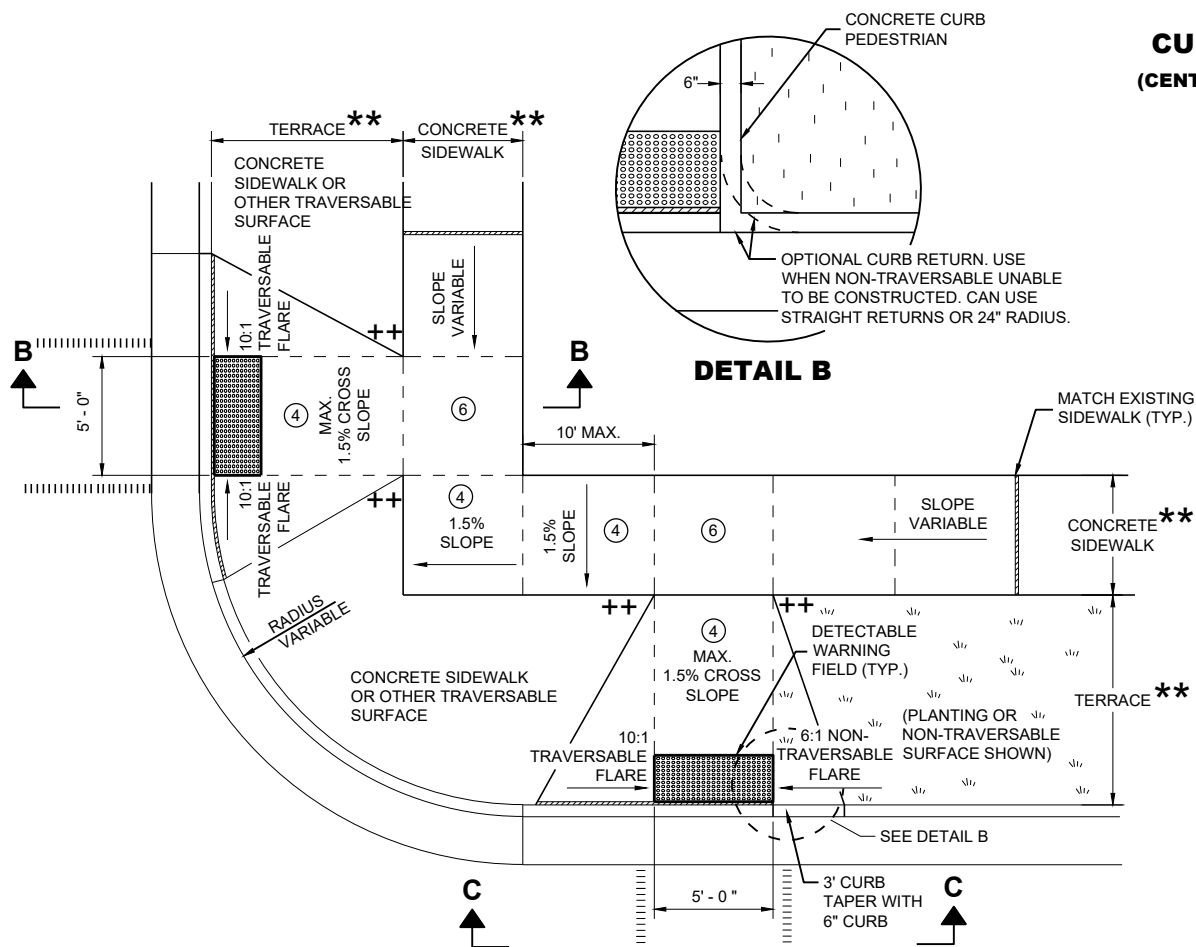
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



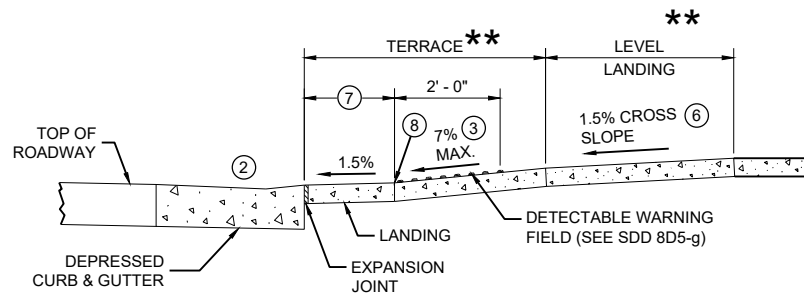
DETAIL A



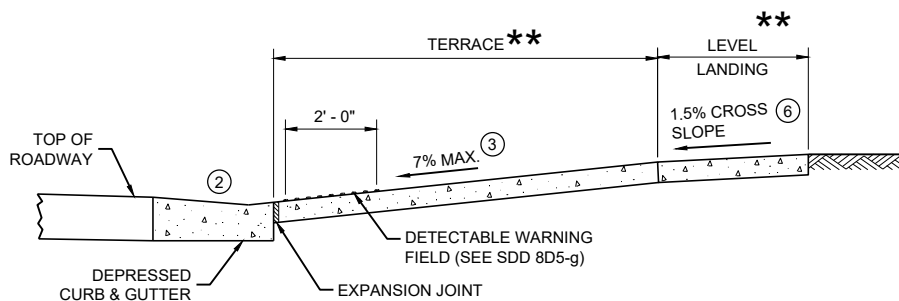
PLAN VIEW
CURB RAMP TYPE 2
(CENTER OF CORNER RADIUS)



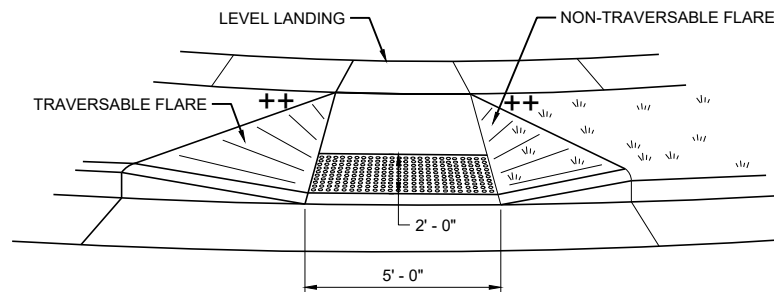
PLAN VIEW
CURB RAMP TYPE 3
(OUTSIDE OF CROSSWALK AREA)



SECTION A - A FOR TYPE 2



SECTION B - B FOR TYPE 3



VIEW C - C FOR TYPE 3

GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE SHALL BE FROM THE SAME MANUFACTURER.
- GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
 - AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE (2.67% OR LESS) AND NOT TO EXCEED 11% GRADE CHANGE.
 - ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
 - PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET X 5 FEET.
 - WHEN GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
 - PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
 - WHEN DISTANCE IS LESS THAN 6' - 0", IT MAY BE DIFFICULT TO ACHIEVE A 7% SLOPE OR FLATTER ALONG THE RAMP. REDUCE CURB HEIGHT IN TRIANGLE AREA TO ACHIEVE 7% SLOPE OR FLATTER ON RAMP. CONSTRUCT 2-INCH MINIMUM CURB HEIGHT BETWEEN 10:1 FLARES.

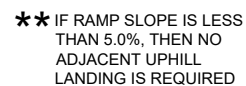
- ★ MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK
- ★★ WIDTH SHOWN ELSEWHERE IN THE PLANS
- ++ CONSTRUCT 6" WEDGE TO AVOID CONCRETE BREAKAGE

LEGEND

- 1/2" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT SIDEWALK
- PAVEMENT MARKING CROSSWALK (WHITE)

CURB RAMPS
TYPE 2 AND 3

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



SECTION B - B FOR TYPE 4A AND TYPE 4A1

RADIUS (AT CURB FACE)	X
10 FEET	4' - 7"

INTERMEDIATE RADII CAN BE INTERPOLATED



GENERAL NOTES




AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN ¼ - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑦ WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑩ INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

LEGEND

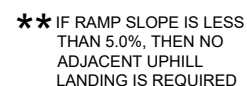
- | | |
|---|------------------------------------|
|  | 1/2" EXPANSION JOINT SIDEWALK |
|  | CONTRACTION JOINT SIDEWALK |
|  | PAVEMENT MARKING CROSSWALK (WHITE) |



++ CONSTRUCT 6" WEDGE TO
AVOID CONCRETE BREAKAGE

CURB RAMPS TYPE 4A AND 4A1

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



**SECTION B - B FOR
TYPE 4B AND TYPE 4B1**

INTERMEDIATE RADII CAN BE INTERPOLATED
 DIMENSION "Y" IS CALCULATED BASED ON 6'-0" RAMP LENGTH
 DIMENSION "X" IS CALCULATED BASED ON 5'-0" SIDEWALK WIDTH



++ CONSTRUCT 6" WEDGE TO
AVOID CONCRETE BREAKAGE

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

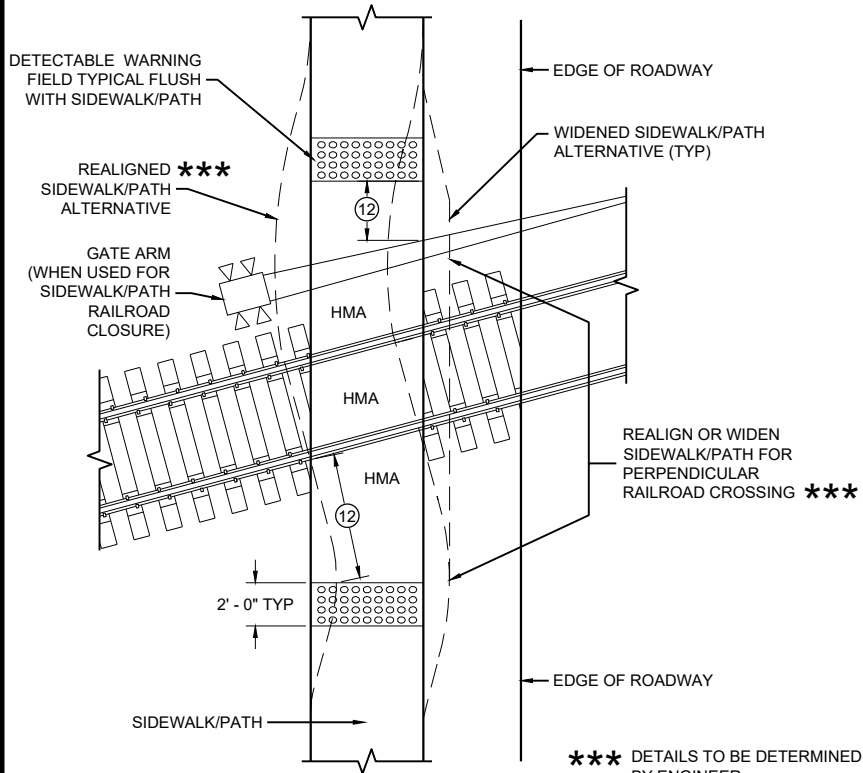
DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4" INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑦ WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑩ INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

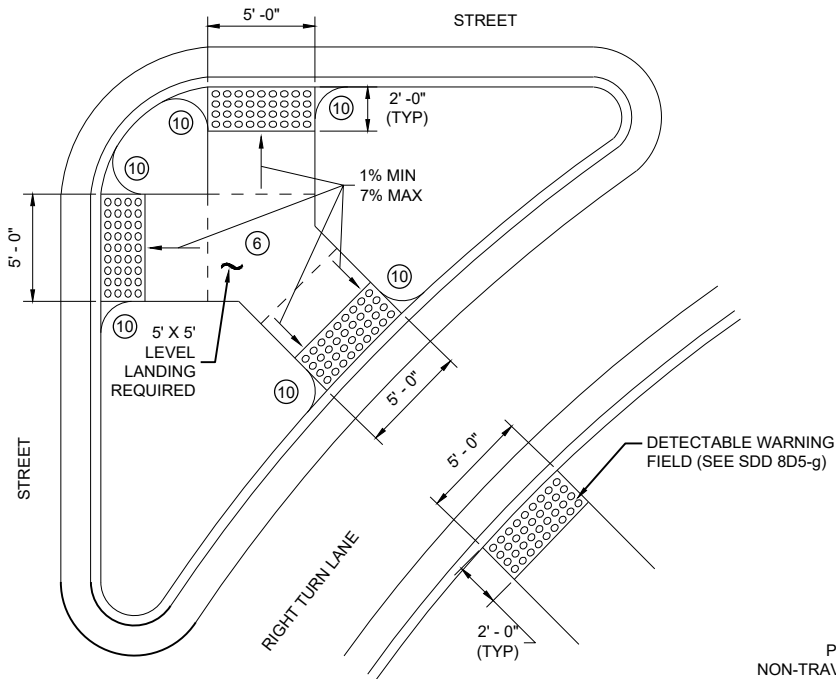


CURB RAMPS TYPE 4B AND 4B1

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

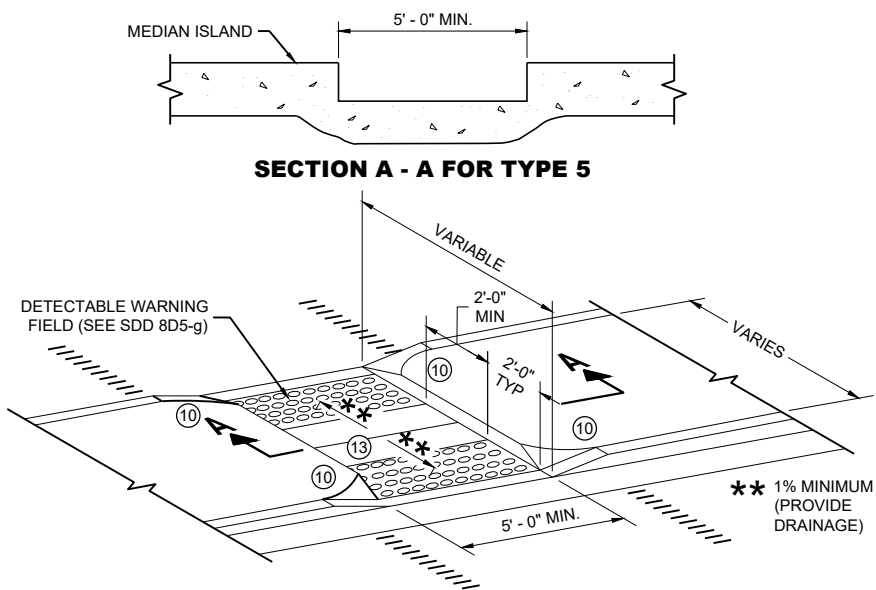


CURB RAMP TYPE 8
DETECTABLE WARNINGS
FOR SIDEWALKS OR SHARED USE PATHS
AT RAILROAD CROSSINGS

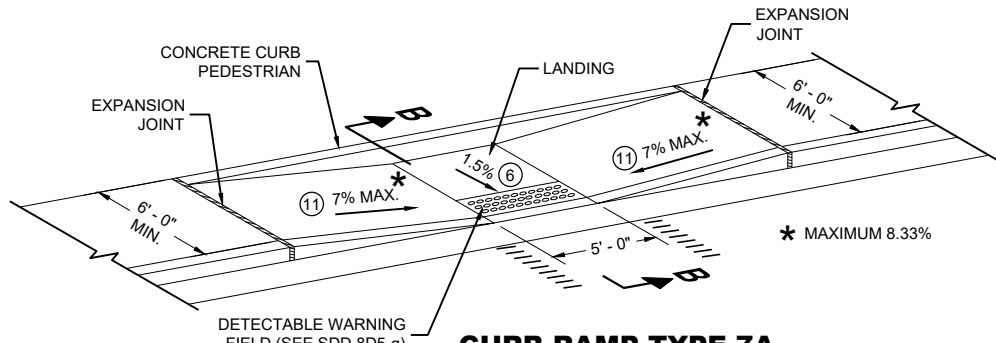


CURB RAMP TYPE 6
DETECTABLE WARNING AT ISLANDS

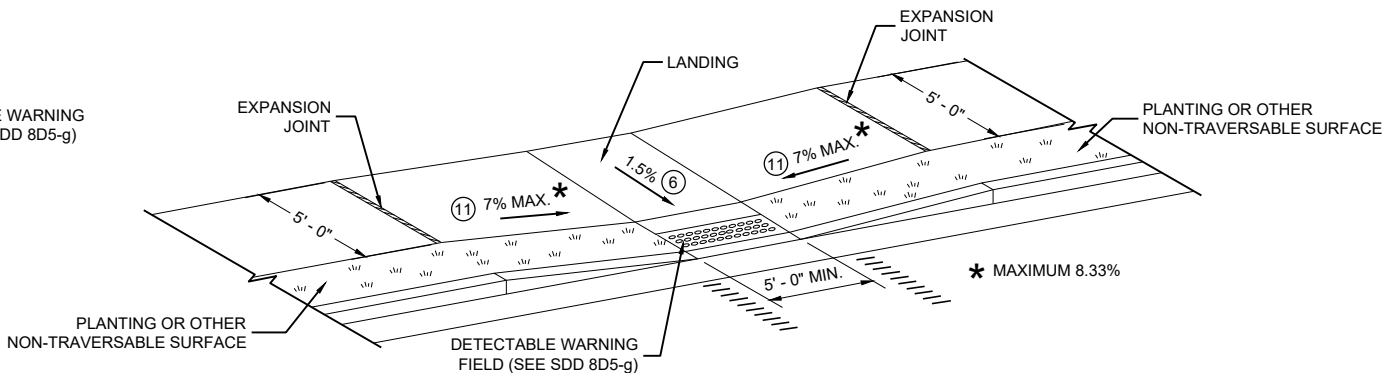
REFER TO GENERAL NOTES ② AND ③
FOR ALL ISLAND CURB RAMPS



CURB RAMP TYPE 5
MEDIAN ISLAND
NON-ELEVATED PEDESTRIAN CROSSING



CURB RAMP TYPE 7A
FOR INTERSECTIONS AND
MID BLOCK CROSSINGS



CURB RAMP TYPE 7B
FOR INTERSECTIONS AND
MID BLOCK CROSSINGS

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

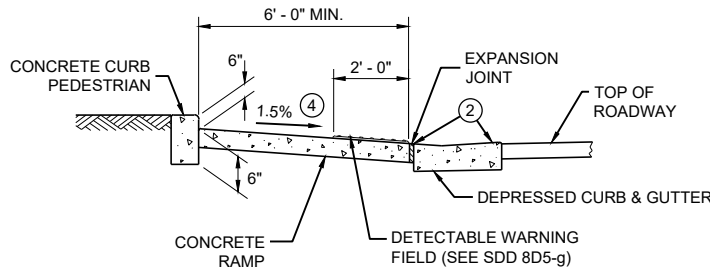
SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/8 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑩ INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.
- ⑪ SLOPE SIDEWALK TOWARD LANDING AS SHOWN WHERE THERE IS NO TERRACE OR WHERE THE TERRACE WIDTH IS LESS THAN 6 FEET WIDE.
- ⑫ THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO A RAILROAD CROSSING SHALL BE 1.5 FEET ±0.1' FROM THE FACE OF THE GATE ARM IF THE GATE ARM EXTENDS ACROSS THE SIDEWALK/PATH. WHERE THERE IS NO PEDESTRIAN GATE, THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO THE RAILROAD TRACK IS 15 FEET MAXIMUM AND 12 FEET MINIMUM, 15 FEET TYPICAL FROM THE NEAREST RAIL.
- ⑬ DO NOT INSTALL DETECTABLE WARNING FIELDS AT THE EDGES OF STEET-LEVEL PEDESTRIAN REFUGE ISLANDS IF A MINIMUM 2 FOOT CONCRETE SURFACE WITHOUT DETECTABLE WARNINGS (MEASURED IN THE DIRECTION OF PEDESTRIAN TRAVEL) CANNOT BE ACHIEVED.

LEGEND

- ===== 1/2" EXPANSION JOINT SIDEWALK
- - - - - CONTRACTION JOINT FIELD LOCATED
- ||||||| PAVEMENT MARKING CROSSWALK (WHITE)

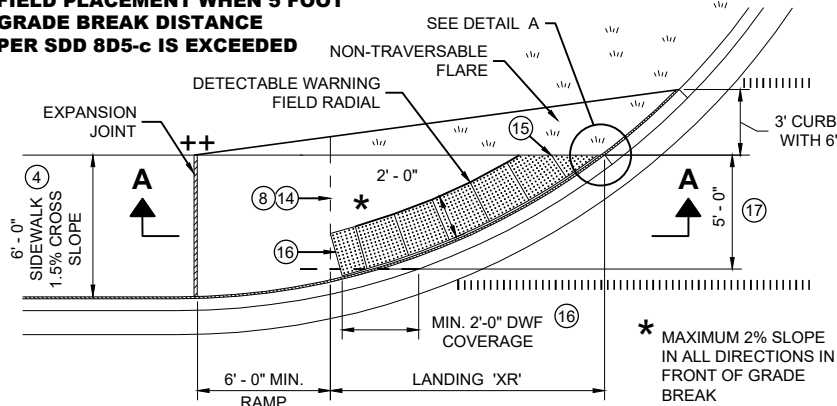


SECTION B - B FOR TYPE 7A

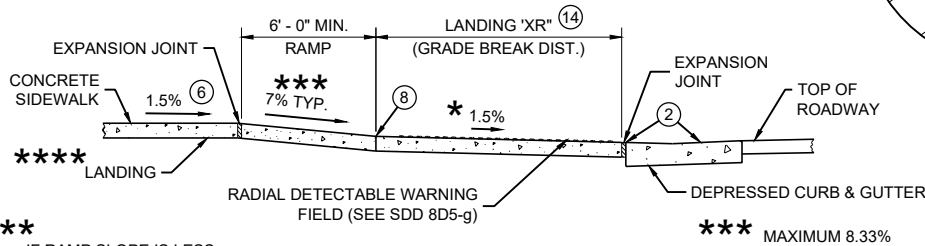
CURB RAMPS
TYPE 5, 6, 7A, 7B & 8

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

**RADIAL DETECTABLE WARNING
FIELD PLACEMENT WHEN 5 FOOT
GRADE BREAK DISTANCE
PER SDD 8D5-c IS EXCEEDED**



**PLAN VIEW
CURB RAMP TYPE 4A1
(GRADE BREAK DISTANCE GREATER THAN 5 FEET)**

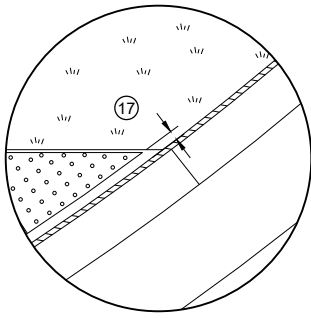


SECTION A - A FOR TYPE 4A1

IF RAMP SLOPE IS LESS
THAN 5.0%, THEN NO
ADJACENT UPHILL
LANDING IS REQUIRED

LEGEND

- 1/2" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT SIDEWALK
- PAVEMENT MARKING CROSSWALK (WHITE)



DETAIL A

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

APPLY RADIAL DETECTABLE WARNING PLACEMENT SIMILARLY FOR TYPE 4A AND 4A1 CURB RAMPS AND SIMILARLY FOR TYPE 4B AND 4B1 CURB RAMPS. TYPE 4A AND 4B CURB RAMPS ARE NOT SHOWN.

REFER TO SDD 8D5-g FOR ADDITIONAL RADIAL PLATE REQUIREMENTS.

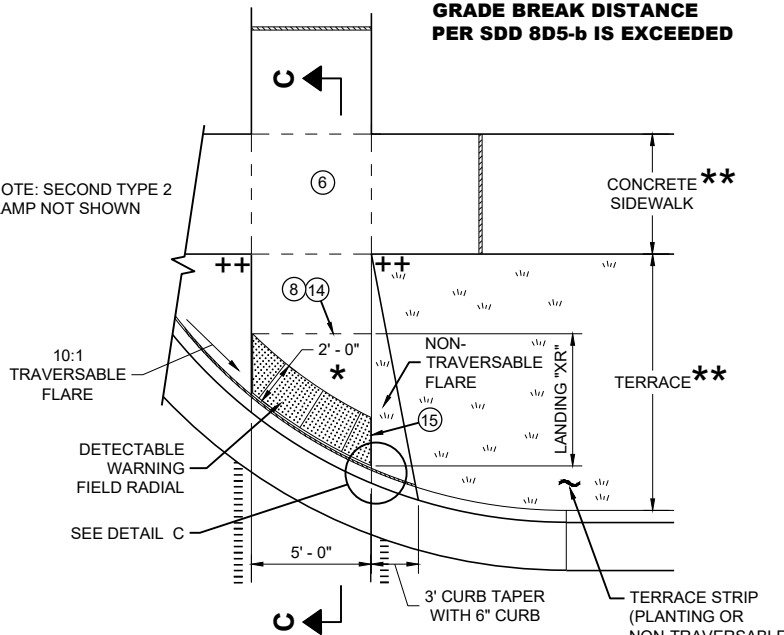
FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FIELD ARE PROHIBITED.

DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.

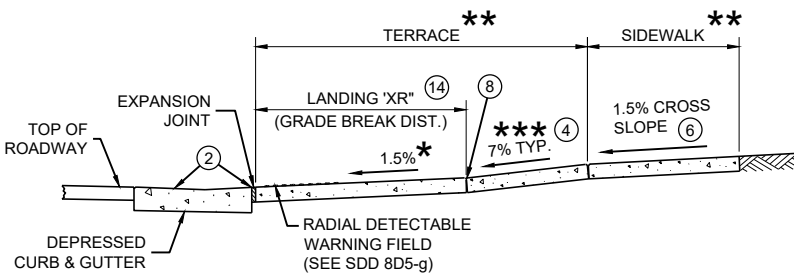
- 2 GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4" INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- 3 AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- 4 ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- 6 PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET BY 5 FEET.
- 8 PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- 14 CONSULT ENGINEER IF GRADE BREAK LOCATION (END OF LANDING DIMENSION "XR") REQUIRES FIELD ADJUSTMENT WHEN ESTABLISHING FINAL RADIAL DETECTABLE WARNING FIELD LOCATION.
- 15 FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN 1/2" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.
- 16 USE 1' X 2" RECTANGULAR END PLATE AT END OF TYPE 4A1 RAMP AND PROVIDE MINIMUM 2' - 0" DETECTABLE WARNING FIELD COVERAGE (IN DIRECTION OF PEDESTRIAN TRAVEL) ALONG THE ENTIRE CURB RAMP WIDTH.
- 17 A MAXIMUM 3 INCH CONCRETE BORDER WIDTH IS ALLOWABLE IN FRONT OF RADIAL DETECTABLE WARNING FIELD FOR CONSTRUCTABILITY PURPOSES. CONCRETE BORDER WIDTH MAY VARY UP TO 1 INCH.

**RADIAL DETECTABLE WARNING
FIELD PLACEMENT WHEN 5 FOOT
GRADE BREAK DISTANCE
PER SDD 8D5-b IS EXCEEDED**

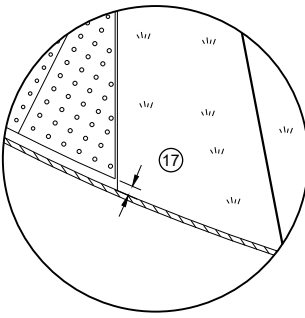
NOTE: SECOND TYPE 2
RAMP NOT SHOWN



**PLAN VIEW
CURB RAMP TYPE 2
(GRADE BREAK DISTANCE GREATER THAN 5 FEET)
(ON LINE WITH SIDEWALK)**



SECTION C - C FOR TYPE 2



DETAIL C

*** MAXIMUM 2% SLOPE
IN ALL DIRECTIONS IN
FRONT OF GRADE
BREAK

** WIDTH SHOWN ELSEWHERE
IN THE PLANS

*** MAXIMUM 8.33%

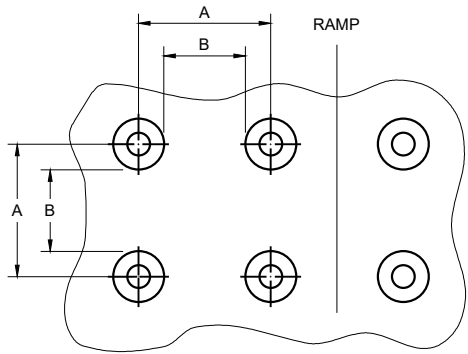
++ CONSTRUCT 6" WEDGE TO
AVOID CONCRETE BREAKAGE

**CURB RAMPS
RADIAL DETECTABLE WARNING**

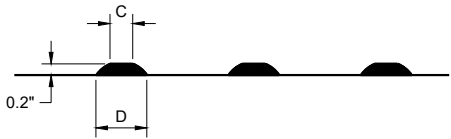
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

	MIN.	MAX.
A	1.6"	2.4"
B	0.65"	1.5"
C	*	*
D	0.9"	1.4"

* THE C DIMENSION IS 50% TO 65% OF THE D DIMENSION.

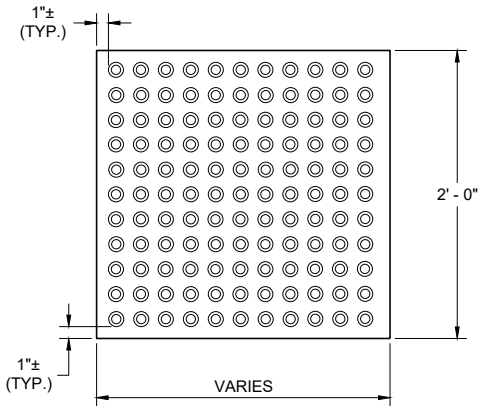


PLAN VIEW

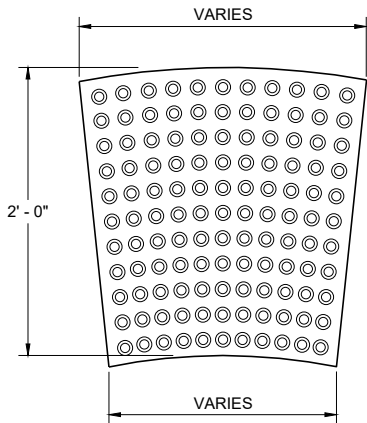


ELEVATION VIEW

TRUNCATED DOMES
DETECTABLE WARNING PATTERN DETAIL

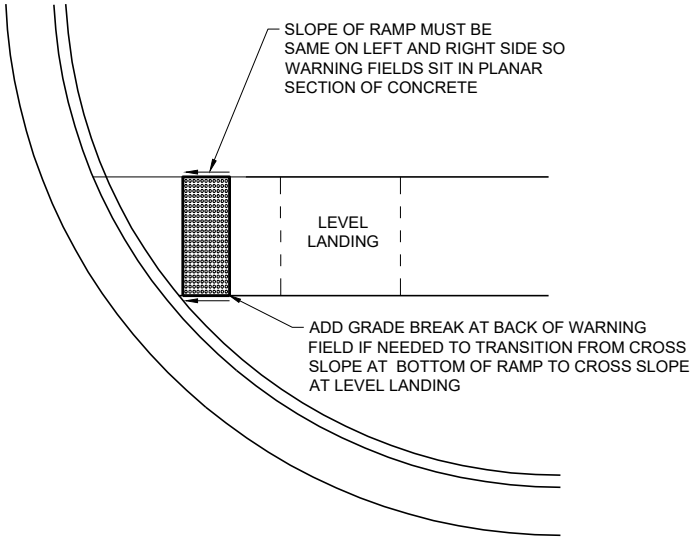


RECTANGULAR
PLATES

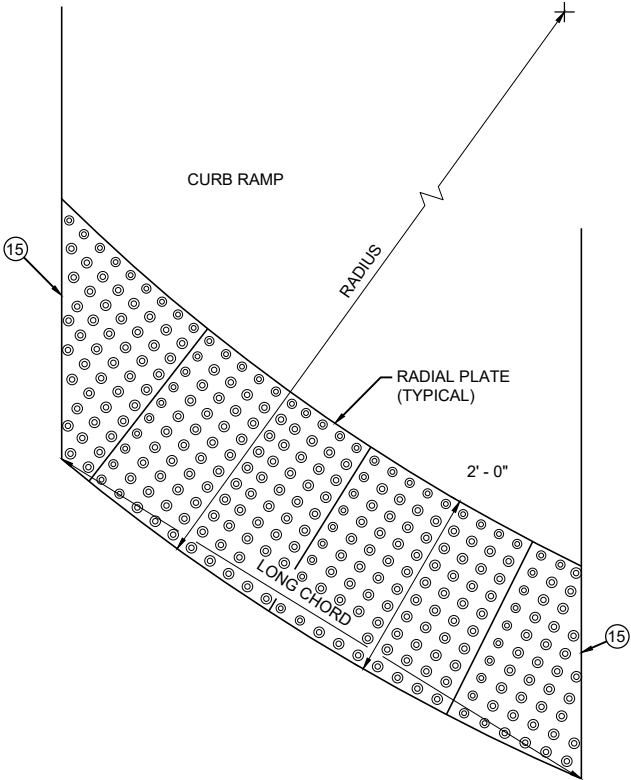


RADIAL
PLATES

PLAN VIEW
DETECTABLE WARNING FIELDS (TYPICAL)



DETECTABLE WARNING FIELD
PLANAR INSTALLATION



PLAN VIEW
RADIAL DETECTABLE
WARNING FIELD ATTRIBUTES

GENERAL NOTES

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AT A CURB RAMP SHALL BE FROM THE SAME MANUFACTURER.

PLACE ALL DETECTABLE WARNING FIELD SYSTEMS IN ACCORDANCE TO THE MANUFACTURER'S RECOMMENDATION.

FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FILED ARE PROHIBITED.

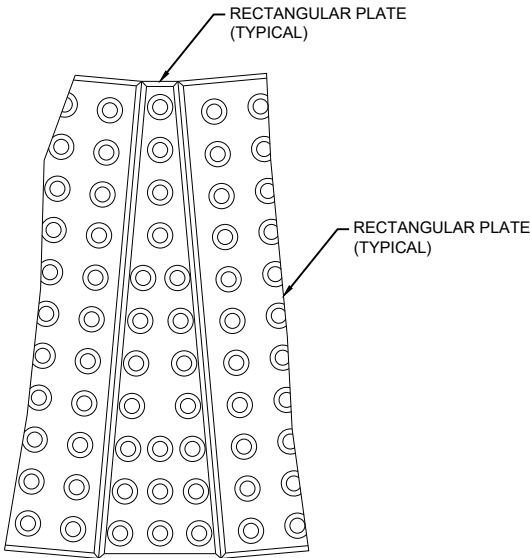
DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.

FOR RADIAL DETECTABLE WARNING FIELD APPLICATIONS WHERE STANDARD RADIAL PLATES ARE NOT AVAILABLE AT AN INTERSECTION CURB RADIUS, A COMBINATION OF SQUARE OR RECTANGULAR PLATES AND RADIAL PLATES MAY BE USED TO FORM RADIAL CONFIGURATION. RADIAL WEDGE PLATES IN COMBINATION WITH SQUARE PLATES ARE ALSO ACCEPTABLE. FOLLOW MANUFACTURER'S RECOMMENDATIONS.

REFER TO CONTRACT AND STANDARD SPECIFICATIONS FOR FIELD CUTTING REQUIREMENTS.

DO NOT EMBED IN CONCRETE ANY FIELD-CUT PLATES WITH CUT EDGES SHORTER THAN 6 INCHES. CONSULT WITH MANUFACTURER FOR RE-DRILLING AND ANCHORING REQUIREMENTS OF FIELD-CUT PLATES.

15 FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN 1/8" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.



PLAN VIEW
RADIAL WEDGE PLATE
CONNECTION DETAIL

CURB RAMPS
RECTANGULAR AND RADIAL
DETECTABLE WARNING PLATES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
July 2023
DATE /S/ Rodney Taylor
ROADWAY STANDARDS DEVELOPMENT
UNIT SUPERVISOR

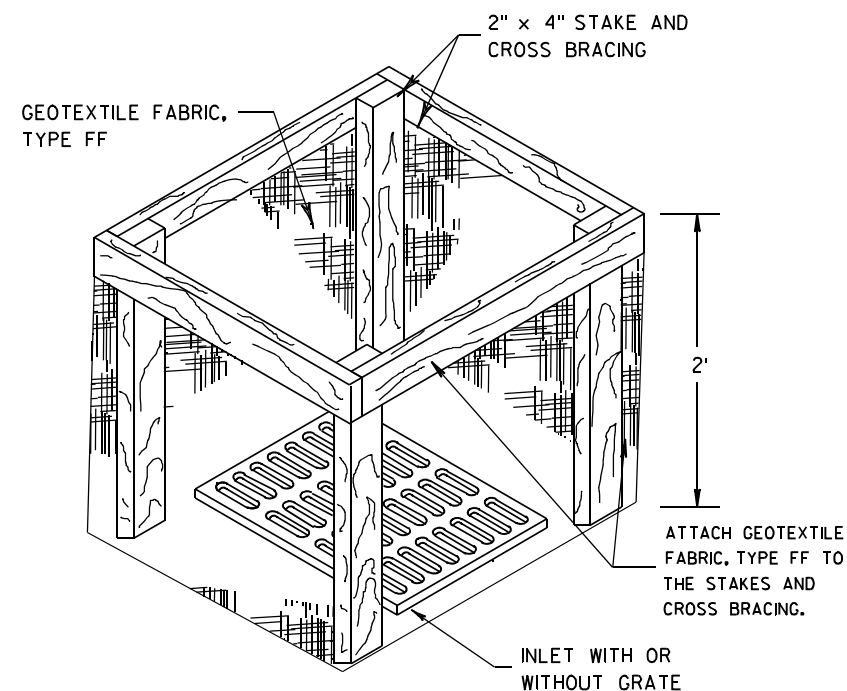
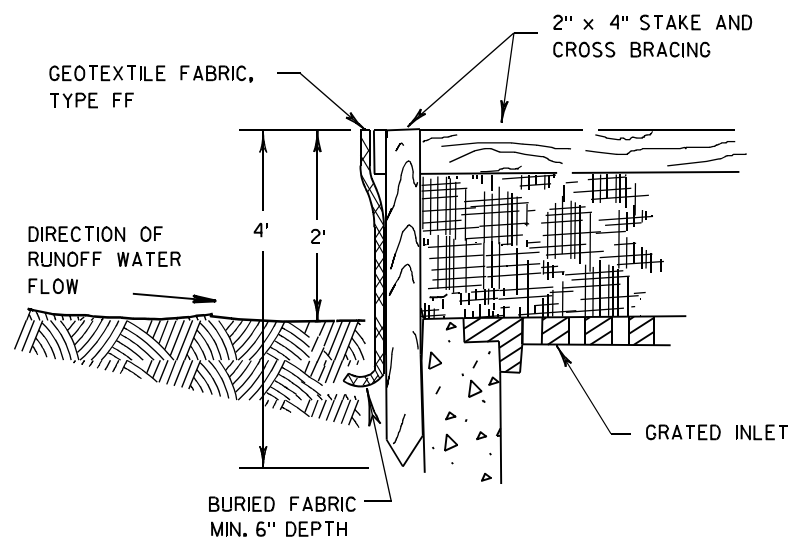
FHWA



- ① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- ③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1 1/8" X 1 1/8" OF OAK OR HICKORY.
- ④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- ⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



<p>SILT FENCE</p>	
<p>STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION</p>	
<p>APPROVED 4-29-05 DATE</p>	<p>/s/ Beth Cannestra CHIEF ROADWAY DEVELOPMENT ENGINEER</p>



INLET PROTECTION, TYPE A

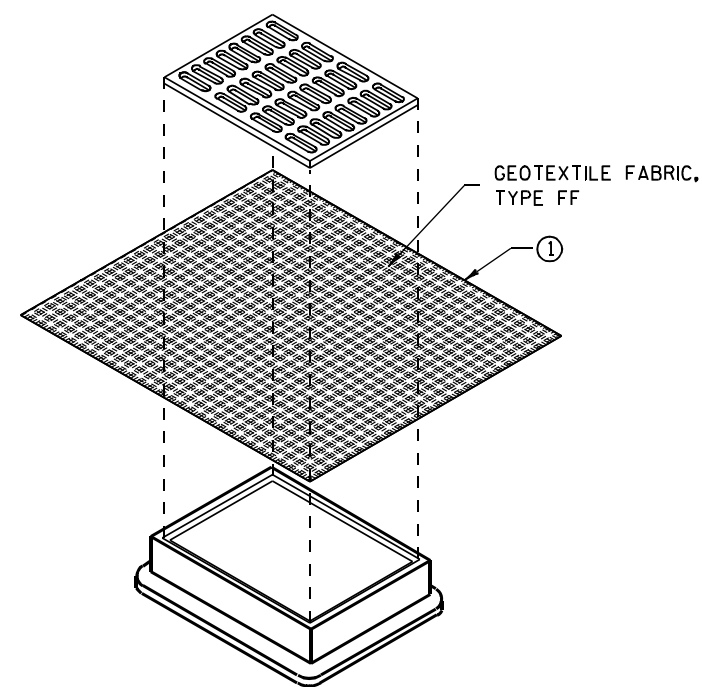
GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

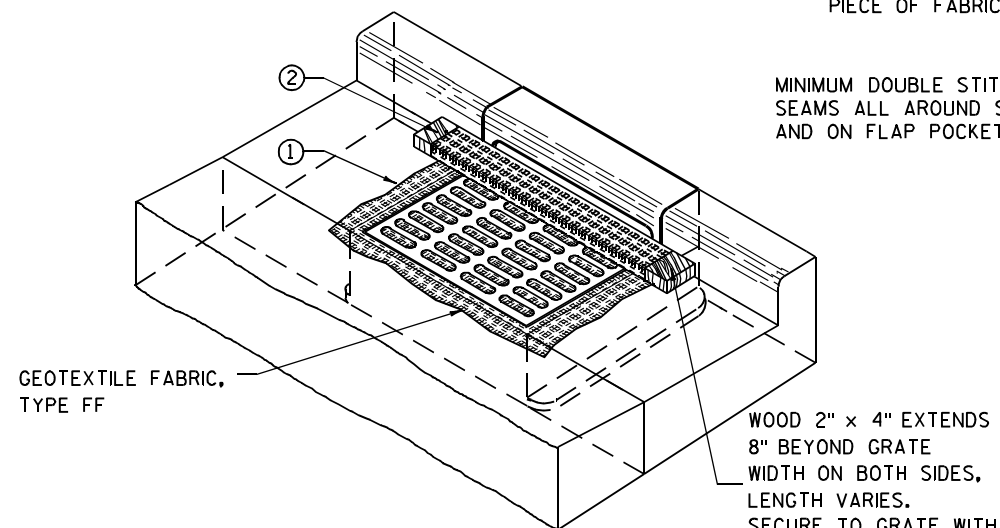
WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- ① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- ② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- ③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



**INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)**

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE C (WITH CURB BOX)

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

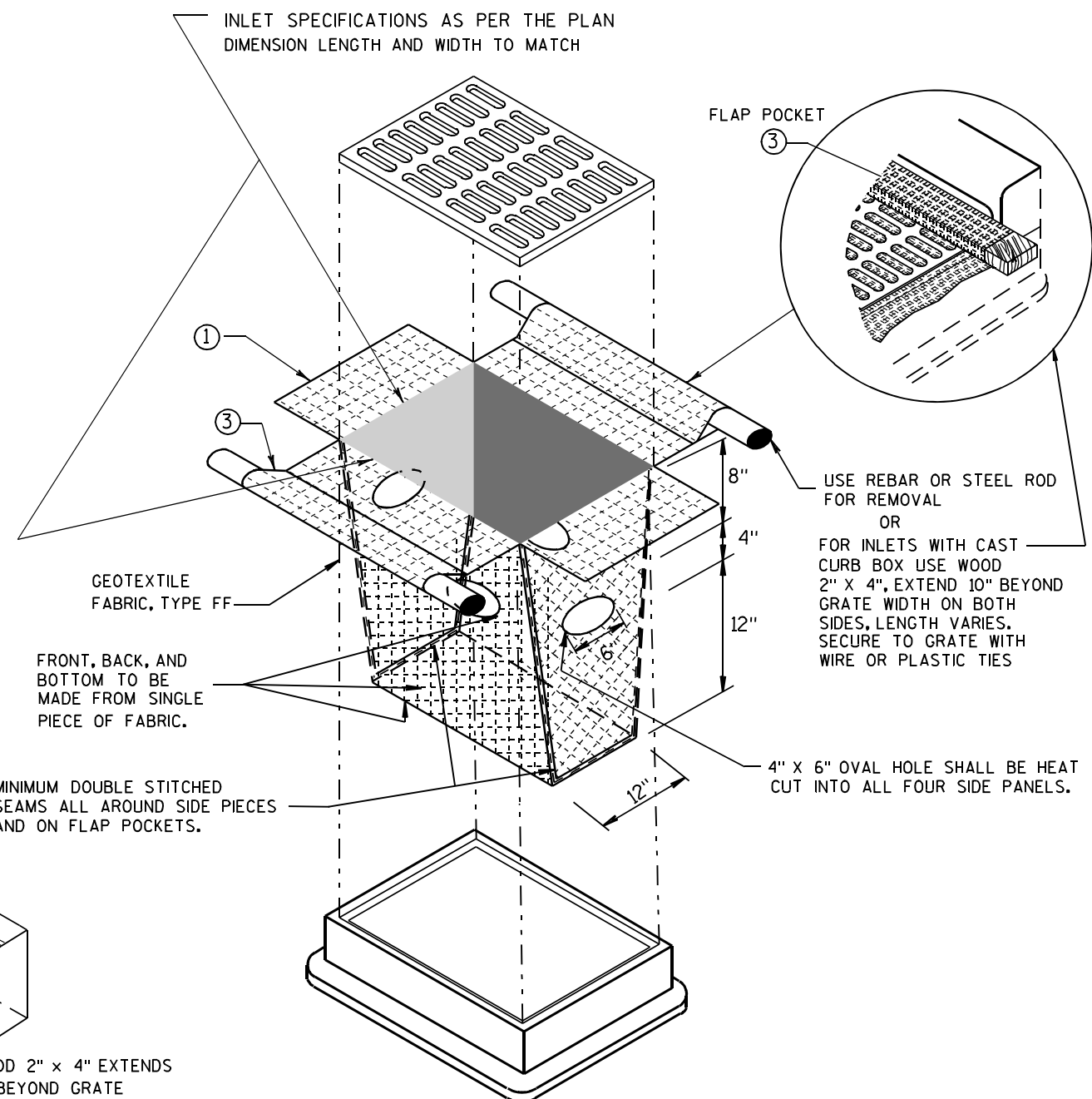
THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

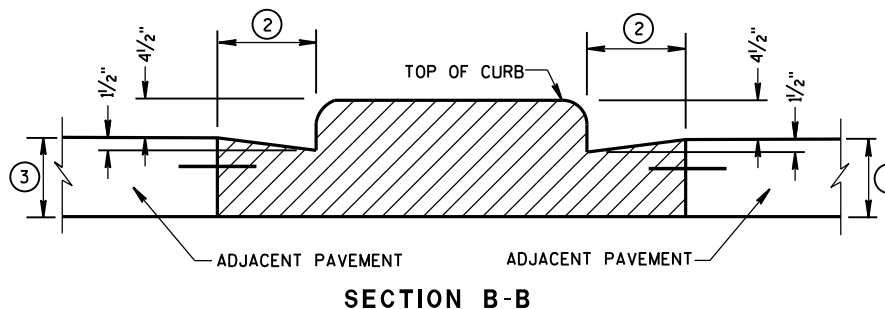
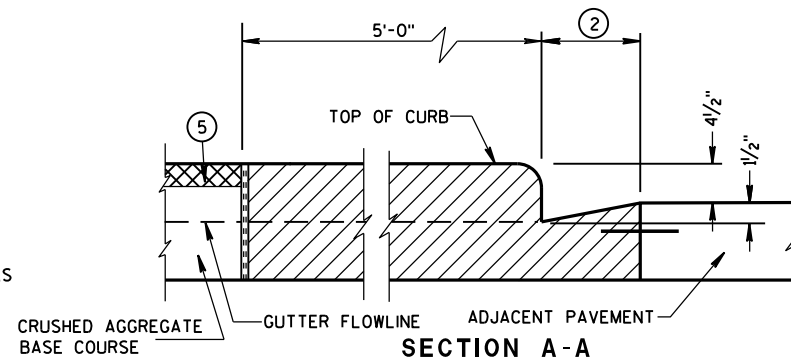
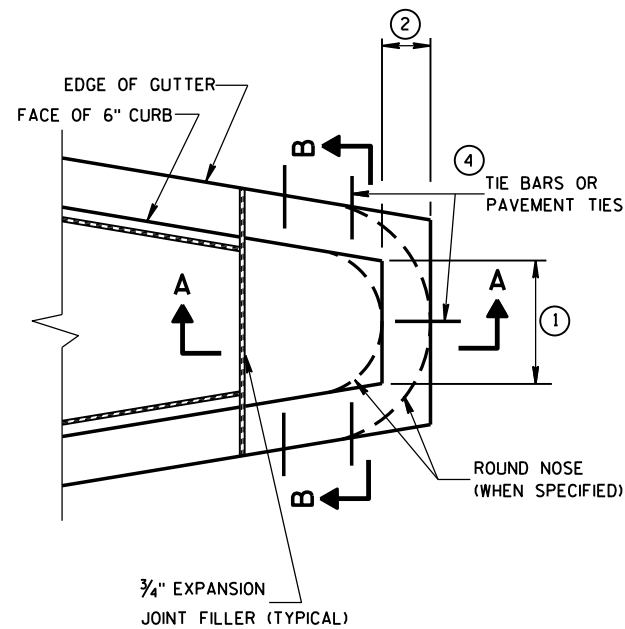
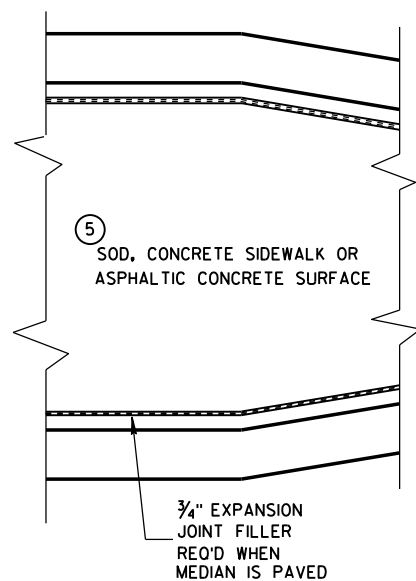
THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.



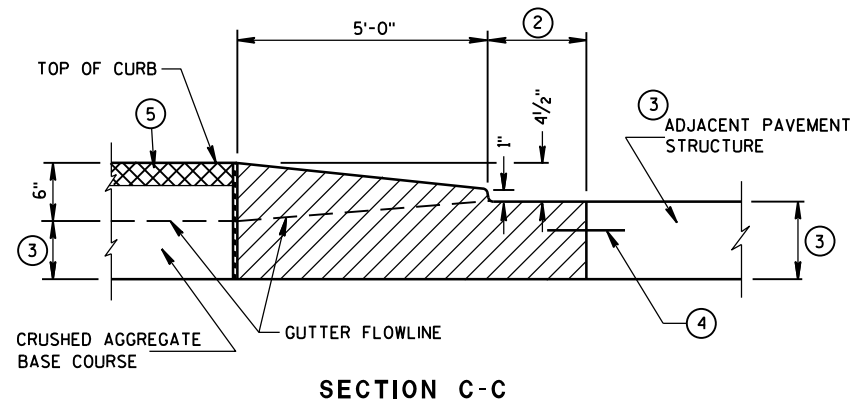
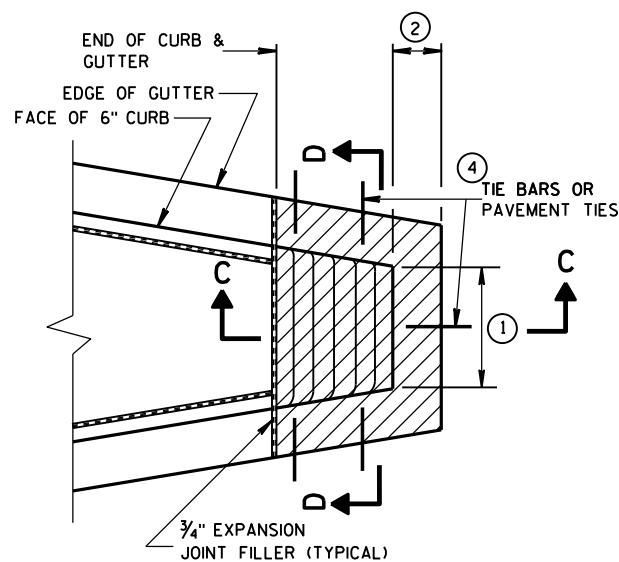
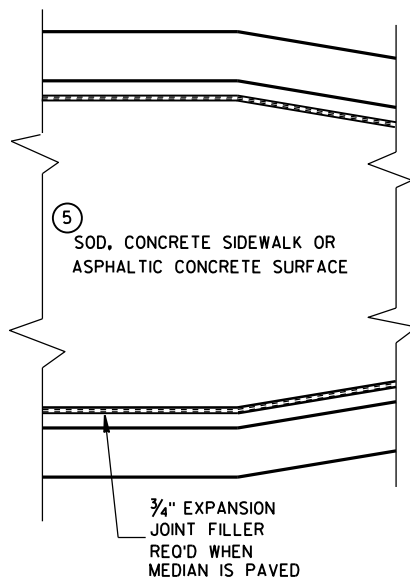
INLET PROTECTION, TYPE D

(CAN BE INSTALLED IN ANY INLET TYPE WITH OR WITHOUT A CURB BOX AS PER NOTE ②)

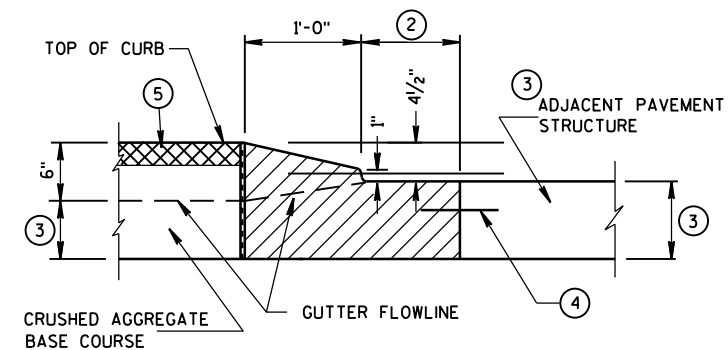
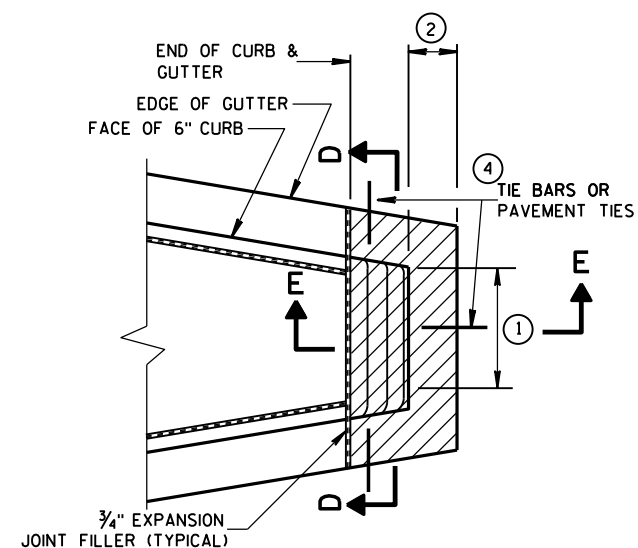
INLET PROTECTION TYPE A, B, C, AND D	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 10/16/02 DATE	/S/ Beth Cannestra CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA	



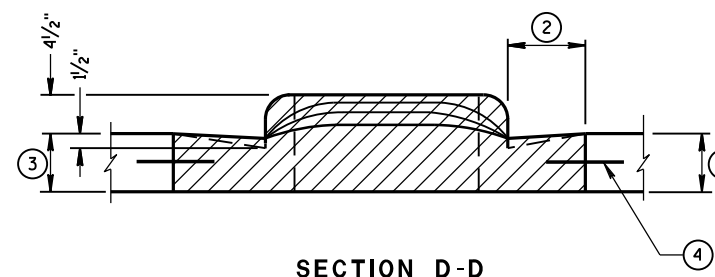
CONCRETE MEDIAN BLUNT NOSE DETAIL



CONCRETE MEDIAN SLOPED NOSE TYPE 1



CONCRETE MEDIAN SLOPED NOSE TYPE 2



GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

- ① SEE PLAN FOR MEDIAN NOSE WIDTH AND RADIUS (FOR ROUND NOSE ALTERNATE).
- ② WIDTH OF GUTTER TO MATCH EXISTING ADJACENT GUTTER OR AS SPECIFIED ELSEWHERE IN THE PLAN.
- ③ DEPTH EQUAL TO ADJACENT PAVEMENT. ADJACENT PAVEMENT STRUCTURE DETAILS ARE SHOWN ON THE PLAN. TYPICAL OPTIONS ARE:
 - (1) NEW OR EXISTING CONCRETE PAVEMENT.
 - (2) ASPHALTIC CONCRETE PAVEMENT OVER NEW OR EXISTING CONCRETE BASE COURSE.
 - (3) ASPHALTIC CONCRETE PAVEMENT OVER CRUSHED AGGREGATE BASE COURSE.

- ④ TIE BARS OR PAVEMENT TIES REQUIRED IN NEW CONCRETE PAVEMENT OR CONCRETE BASE COURSE. TIE BARS SHALL BE NO. 4 X 2'-0" SPACED AT 2'-0" C-C.

PAVEMENT TIES REQUIRED IN EXISTING CONCRETE BASE COURSE. PAVEMENT TIES SHALL BE NO. 6 X 1'-0" SPACED AT 3'-0" C-C INSTALLED ON A HORIZONTAL SKEW OF 6:1. THE DIRECTION OF SKEW SHALL ALTERNATE AFTER EVERY ONE OR TWO BARS.

- ⑤ SURFACE TYPE AND DETAILS ARE SHOWN ELSEWHERE IN THE PLAN.

CONCRETE MEDIAN NOSE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

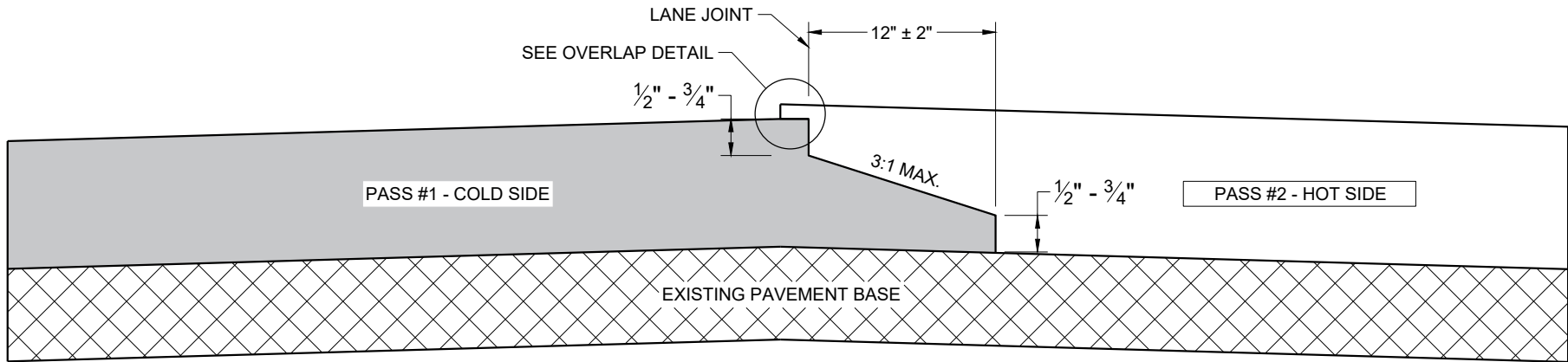
APPROVED

6/8/2006

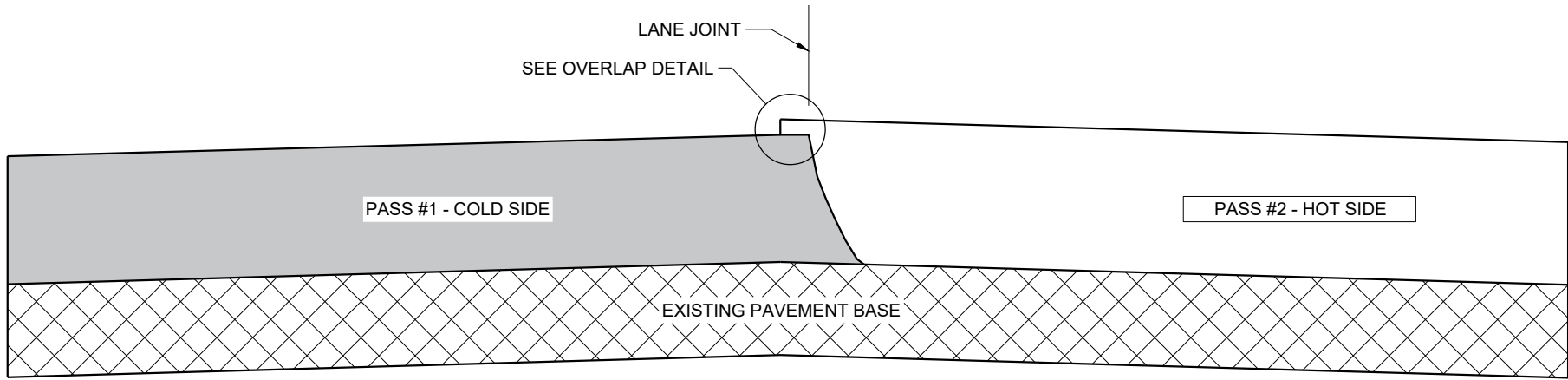
DATE

FHWA

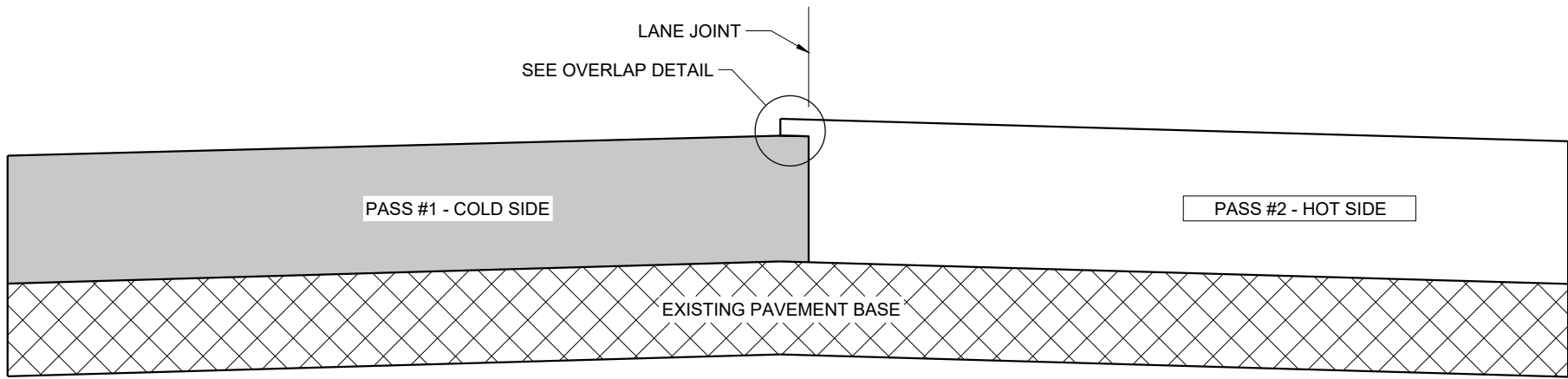
/S/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT
ENGINEER



TYPICAL PAVEMENT CROSS SECTION
NOTCHED WEDGE JOINT



TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT



TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT (MILLED)

GENERAL NOTES

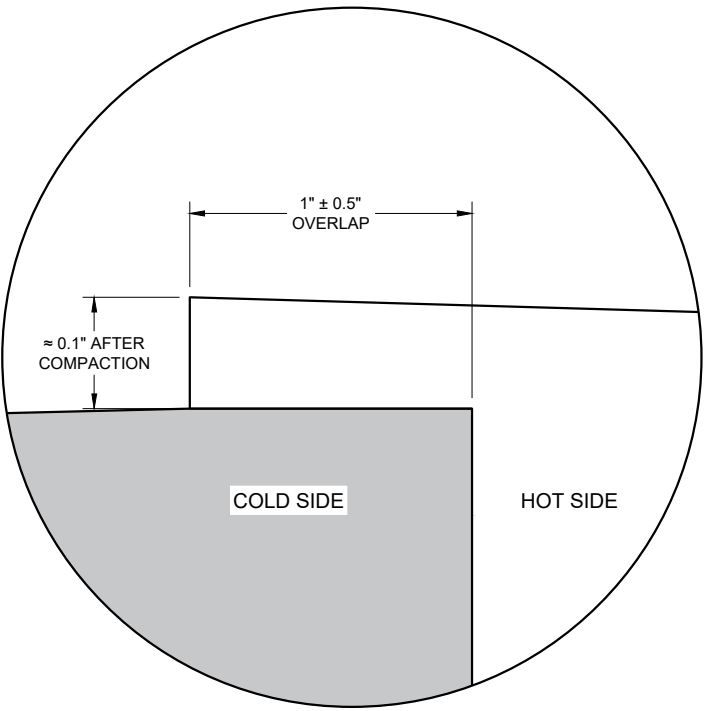
IN ADDITION TO THE DETAILS PROVIDED IN THIS DRAWING, CONFORM TO STANDARD SPECIFICATION 450.3.2.8 FOR WHEN A NOTCHED WEDGE JOINT IS REQUIRED AND FOR GENERAL JOINT CONSTRUCTION REQUIREMENTS.

FOR ALL LONGITUDINAL JOINTS, ENSURE THE PAVER SCREED OVERLAPS THE PREVIOUSLY PLACED PAVEMENT BY 1" ± 0.5" AND THE HOT SIDE OF THE JOINT REMAINS HIGHER THAN THE COLD SIDE BY APPROXIMATELY 0.1" AFTER FINAL COMPACTION. (IT WILL BE FLUSH WHEN PAVING IN ECHELON.)

ONLY REMOVE THE LONGITUDINAL NOTCHED WEDGE JOINT FOR SMA PAVEMENT OR AS DIRECTED BY THE ENGINEER TO ADDRESS SPECIFIC LENGTHS OF JOINT DAMAGED BY TRAFFIC.

WHEN MILLING BACK OR REMOVING ANY LONGITUDINAL JOINT, LIMIT THE MATERIAL REMOVED TO 2" FROM THE TOP NOTCH OR FROM THE VERTICAL JOINT EDGE ON THE COLD SIDE OF THE JOINT.

USE LONGITUDINAL MILLED JOINT AS PLANS SHOW OR THE AS THE ENGINEER DIRECTS.



OVERLAP DETAIL (TYPICAL)

HMA LONGITUDINAL JOINTS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2020 /S/ Steven Hefel
DATE HMA PAVEMENT ENGINEER
FHWA



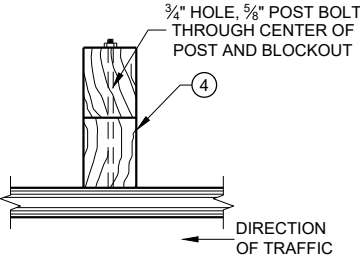
SDD 14B15a Steel Plate Beam Guard, Class "A", Installation and Elements

GENERAL NOTES

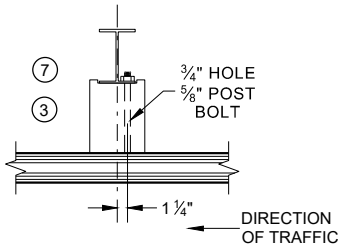
- WOOD OR STEEL POSTS (w6x9 OR w6x8.5) AND NOTCHED PLASTIC BLOCKOUTS ARE ACCEPTABLE ALTERNATIVES FOR 6"x8" WOOD POSTS WITH WOOD OR PLASTIC BLOCKOUTS. USE APPROVED NOTCHED PLASTIC BLOCKOUTS WITH STEEL POSTS. APPROVED PLASTIC BLOCKOUT DESIGNS MAY VARY FROM THIS TYPICAL DETAIL WHEN USED IN CONJUNCTION WITH STEEL POSTS. DO NOT MIX STEEL AND WOOD POSTS IN A SINGLE INSTALLATION.
- USE STRUCTURAL STEEL POSTS CONFORMING TO ASTM A 36. GALVANIZED POSTS ACCORDING TO AASHTO M 111. EITHER SET THE POSTS IN DRILLED HOLES OR DRIVE TO GRADE. REMOVE MUSHROOMING CAUSED BY DRIVING AND REPAIR DAMAGE SPALTER COATING ON GALVANIZED POSTS.
- INSTALL STEEL POSTS WITH HOLES ON APPROACHING TRAFFIC SIDE.
- USE EITHER WOOD OR APPROVED PLASTIC BLOCKOUTS ON WOOD POSTS.
- IF THE DISTANCE FROM BACK OF POST TO SHOULDER HIGHE POINT IS LESS THAN 2 FEET, INSTALL LONGER POST AT HALF POST SPACING, W BEAM (LHW).
- IF ROCK IS ENCOUNTERED DURING EXCAVATION, THE ENGINEER MAY APPROVE USING A 12 INCHES IN DIAMETER POST HOLE EXTENDING 20 INCHES DEEP INTO THE ROCK. PLACE APPROXIMATELY 2 1/2" INCHES OF GRANULAR MATERIAL IN THE BOTTOM OF THE HOLE. CUT THE POSTS THE TO LENGTH AMD INSTALL. BACKFILL WITH EXCAVATED MATERIAL AND COMPACT ADEQUATELY.
- WHEN USING STEEL POSTS AND WOOD BLOCKOUTS, INSTALL FOUR 16d GALVANIZED NAILS. INSTALL NAILS AT THE BACK CORNERS OF THE BLOCK AND BEND THE NAILS OVER THE FLANGE OF THE STEEL POST.

INSTALL BEAM GUARD SECTIONS AND ALL NECESSARY HARDWARE ACCORDING TO THE APPLICABLE PLAN AND CURRENT STANDARD AND SUPPLEMENTAL SPECIFICATIONS.

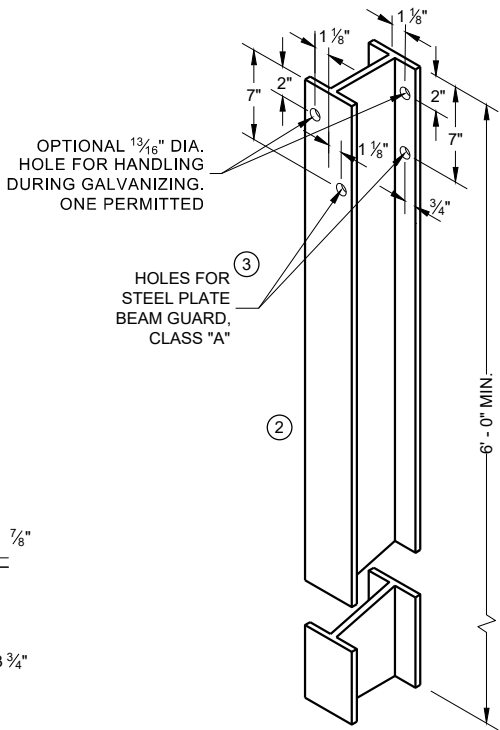
ALL DIMENSIONS ARE SUBJECT TO MANUFACTURER'S TOLERANCES EXCEPT WHERE ALLOWABLE TOLERANCES ARE SHOWN.



PLAN VIEW
WOOD POST, BLOCKOUT AND BEAM

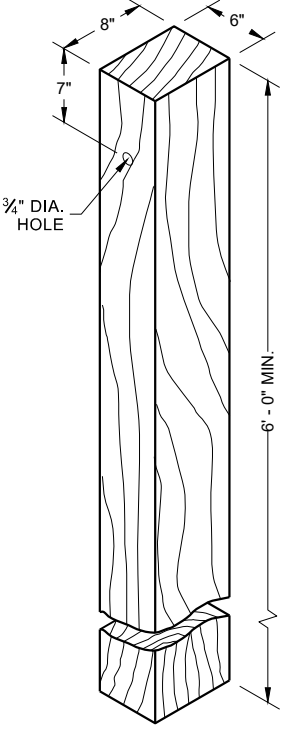


PLAN VIEW
WOOD POST, BLOCKOUT AND BEAM

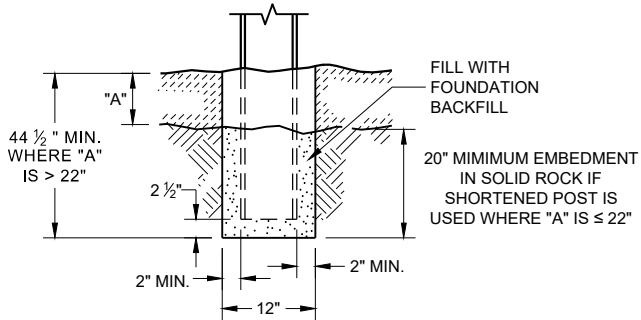


STEEL POST & HOLE PUNCHING DETAIL (W 6 X 9) ①

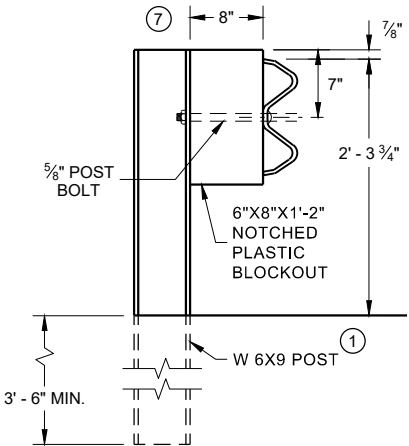
ALL HOLES 13/16" DIAMETER EXCEPT AS NOTED



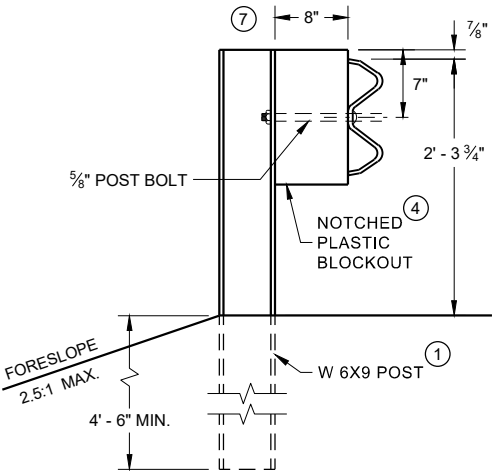
WOOD POST (6" X 8") NOMINAL ①



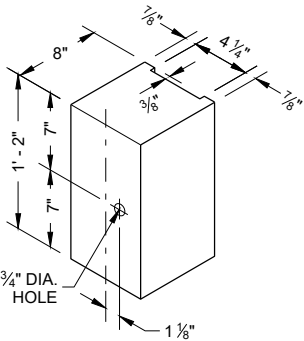
END VIEW
SETTING STEEL OR WOOD POST IN ROCK ⑥



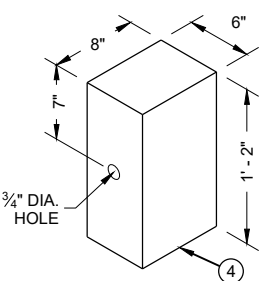
END VIEW
STEEL POST AND NOTCHED PLASTIC BLOCKOUT ALTERNATIVE STANDARD INSTALLATION



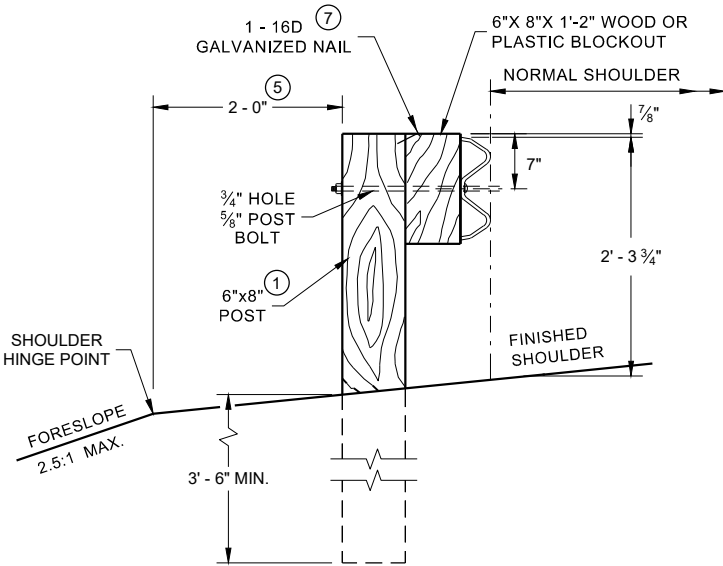
END VIEW
LONGER POST AT HALF POST SPACING W BEAM (LHW)



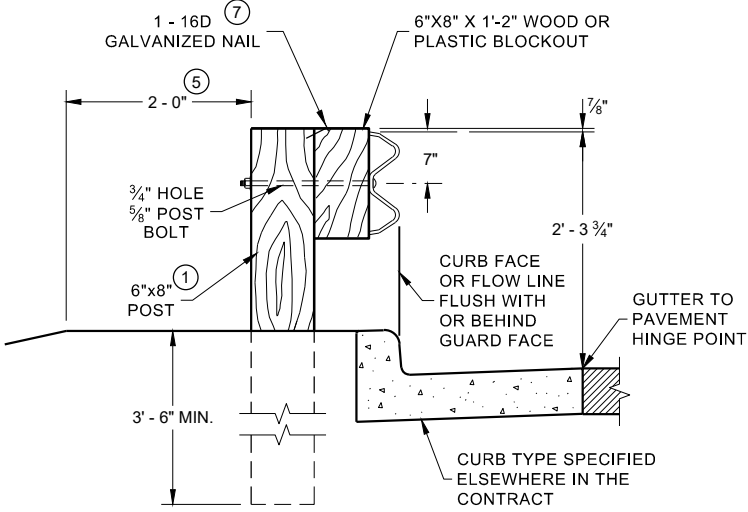
TYPICAL NOTCHED PLASTIC BLOCKOUT FOR STEEL POSTS



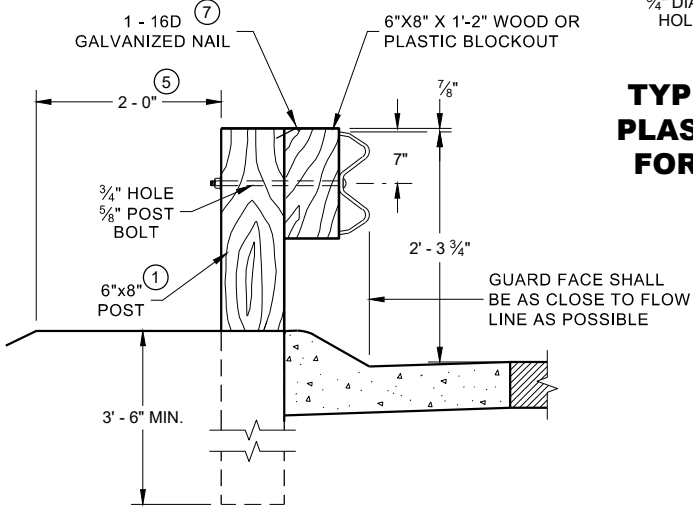
WOOD OR PLASTIC BLOCKOUT FOR WOOD POSTS



END VIEW
LOCATED ALONG A ROADWAY SHOULDER STANDARD INSTALLATION



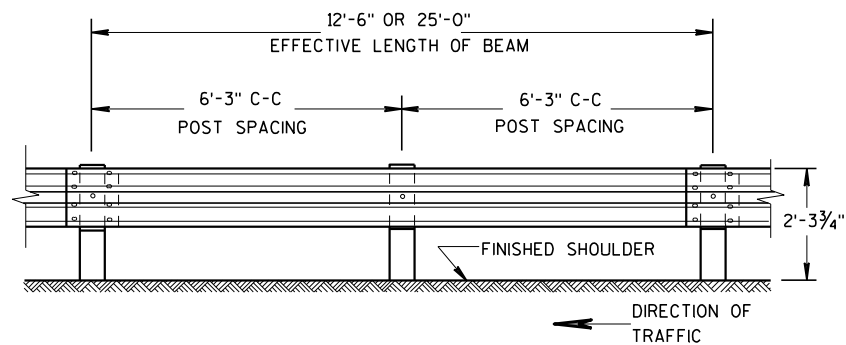
END VIEW
LOCATED ALONG A CURBED ROADWAY



END VIEW
LOCATED ALONG A MOUNTABLE CURBED ROADWAY

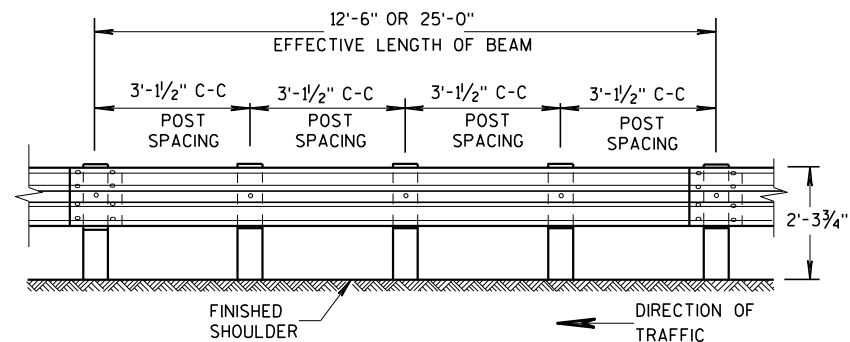
STEEL PLATE BEAM GUARD, CLASS "A" INSTALLATION AND ELEMENTS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

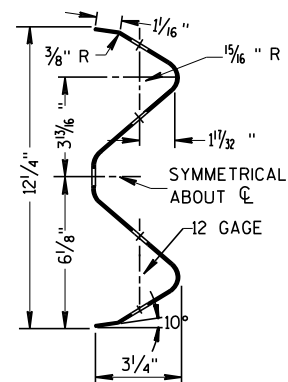


FRONT VIEW

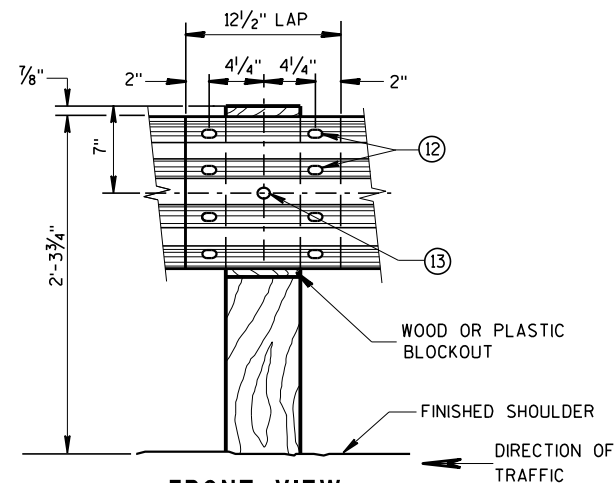
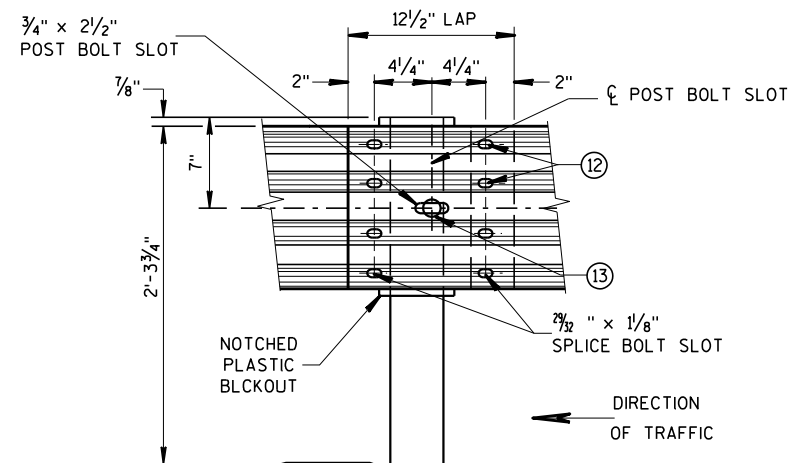
POST SPACING STANDARD INSTALLATION



FRONT VIEW

POST SPACING FOR LONGER POST
AT HALF POST SPACING W BEAM (LHW)

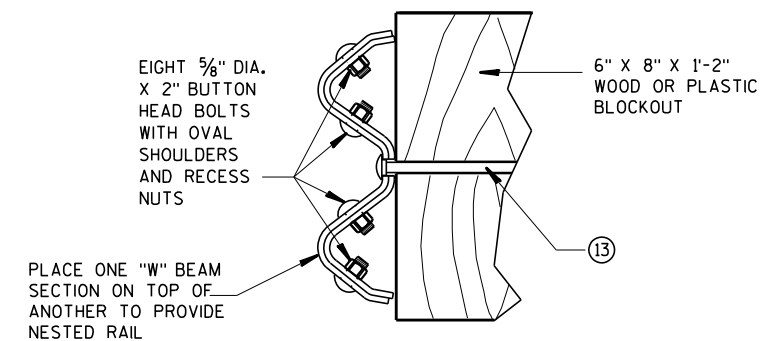
SECTION THRU W BEAM

FRONT VIEW
BEAM SPLICE AT WOOD POST
AND POST MOUNTING DETAILFRONT VIEW
BEAM SPLICE AT STEEL POSTTYPICAL SPLICING DETAILS
OF STEEL PLATE BEAM GUARD

GENERAL NOTES

FURNISH GUARDRAIL DEFLECTORS FROM APPROVED PRODUCTS LIST.

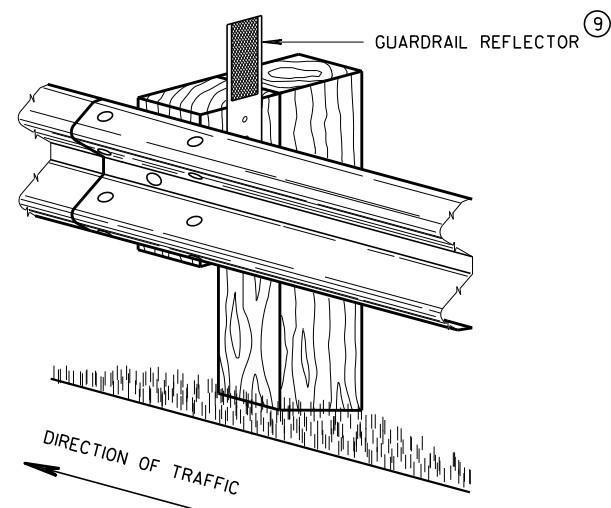
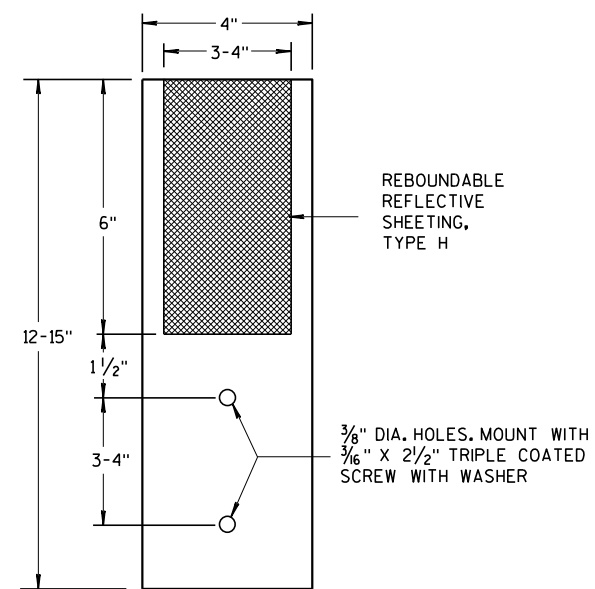
- ⑨ DO NOT INSTALL REFLECTORS ON THE FIRST 50 FEET OF THE APPROACH END OF THE ENERGY ABSORBING TERMINA. START REFLECTORS AT POST #9 AND SPACE EVENLY EVERY 100 FEET (MAX.) TO THE END OF GUARDRAIL RUN, USING A MINIMUM OF 3 REFLECTORS.
- ⑫ 8 - 5/8" ϕ X 2" BUTTON HEAD BOLTS WITH OVAL SHOULDERS & RECESS NUTS.
- ⑬ 5/8" DIA. BUTTON HEAD BOLT AND RECESS NUT WITH 5/8" DIA. F844 FLAT WASHER UNDER NUT.



NESTED W BEAM (NW)

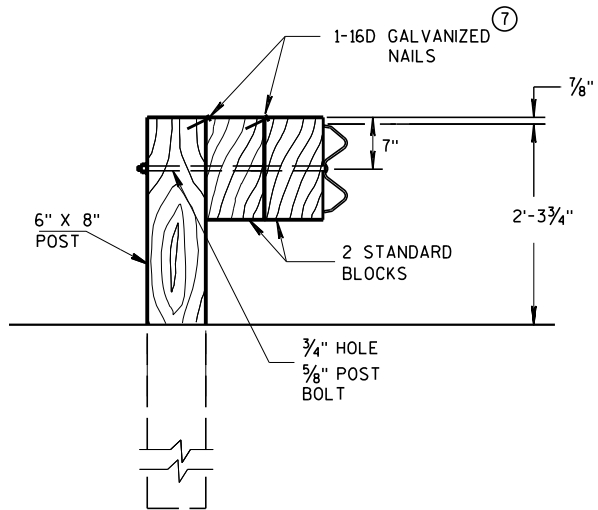
USE ALL OTHER STANDARD BEAM GUARD DETAILS FOR
CONSTRUCTING NESTED W BEAM (NW)

* USE DOUBLE SIDED WHITE GUARDRAIL REFLECTORS ON ROADWAYS WITH BI-DIRECTIONAL TRAFFIC (NO MEDIAN). USE SINGLE SIDED WHITE (RIGHT SIDE) AND SINGLE SIDED YELLOW (LEFT SIDE) ON ROADWAYS WITH MEDIAN SEPARATION.

4" X 12" GUARDRAIL REFLECTOR DETAIL
AND TYPICAL INSTALLATION *

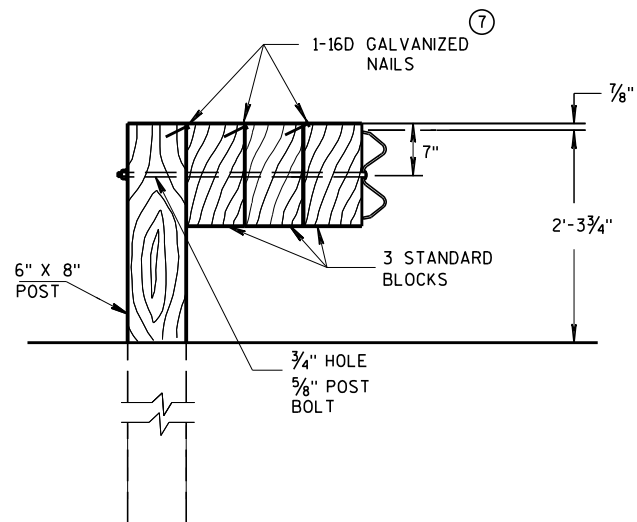
4"x 12" GUARDRAIL REFLECTOR

STEEL PLATE BEAM GUARD,
CLASS "A",
INSTALLATION & ELEMENTSSTATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



DETAIL FOR DOUBLE BLOCKS

THE NUMBER OF DOUBLE BLOCK POSTS
WITHIN A BARRIER RUN IS UNLIMITED

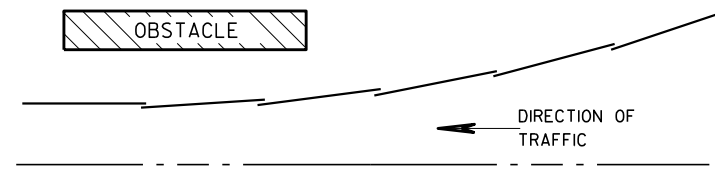


DETAIL FOR TRIPLE BLOCKS

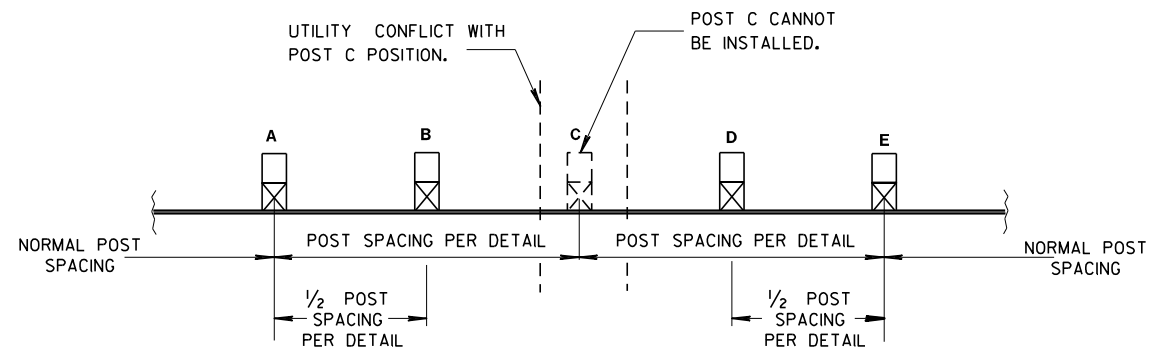
TRIPLE BLOCK DETAIL IS LIMITED TO ONE
LOCATION WITHIN A BEAM GUARD RUN.

NOTES: USE DOUBLE OR TRIPLE BLOCKS WHEN UNDERGROUND OBSTACLES
PREVENT THE POST FROM BEING INSTALLED.

DO NOT USE EXTRA BLOCKOUTS IF IT CAUSES THE POST TO BE DRIVEN BEYOND
SHOULDER HINGE POINT OR CAUSES A FIXED OBJECT TO BE WITHIN THE DEFLECTION
DISTANCE OF THE BARRIER.



PLAN VIEW BEAM LAPPING DETAIL

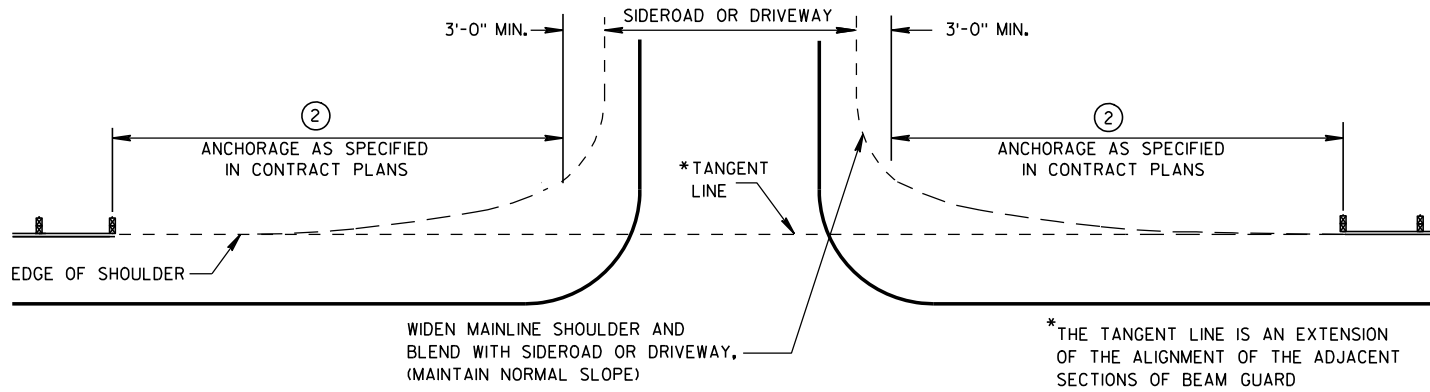


POST DRIVING FOR CONTINUOUS UNDERGROUND OBSTRUCTION

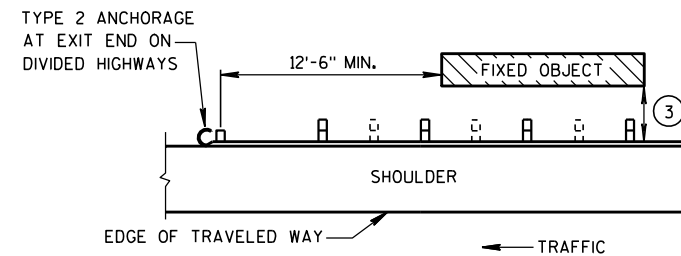
STEEL PLATE BEAM GUARD, CLASS "A", INSTALLATION & ELEMENTS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED	/S/ Rodney Taylor
June 2017	DATE
	ROADWAY STANDARDS DEVELOPMENT
	UNIT SUPERVISOR
FHWA	



BEAM GUARD AT SIDEROADS OR DRIVEWAYS



BEAM GUARD AT OBSTACLES EXIT END - ONE WAY TRAFFIC

GENERAL NOTES

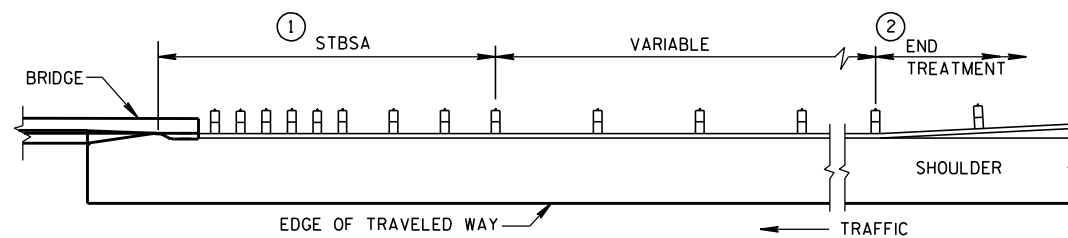
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP SHALL CONFORM TO THE PERTINENT STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

W6 X 9 OR W6 X 8.5 STEEL POSTS WITH NOTCHED PLASTIC BLOCKOUTS ARE ACCEPTABLE ALTERNATIVES FOR 6" X 8" WOOD POSTS WITH WOOD OR PLASTIC BLOCKOUTS. USE APPROVED NOTCHED PLASTIC BLOCKOUTS WITH STEEL POSTS.

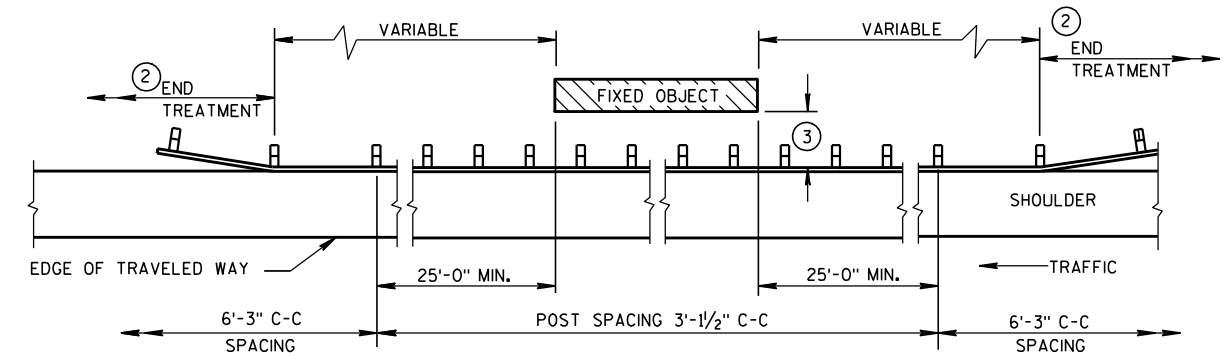
THE LOCATIONS AND LENGTHS OF BEAM GUARD ARE SHOWN ELSEWHERE IN THE PLAN.

- ① STEEL THRIE BEAM STRUCTURAL APPROACH (STBSA) - SEE CURRENT SDD 14B20.
- ② USE AN APPROVED END TREATMENT FOR THE TRAFFIC APPROACH SIDE OF BRIDGE/OBSTACLES. USE TYPE 2 ANCHORAGE ONLY AT THE DOWNSTREAM ENDS OF BEAM GUARD LOCATED ALONG ROADWAYS WITH ONE WAY TRAFFIC.

MINIMUM LATERAL DISTANCE FROM FACE OF BEAM GUARD TO FIXED OBJECT	POST SPACING
3'-6"	3' - 1½"
4'-6"	6' - 3"



BEAM GUARD AT FULL WIDTH BRIDGES

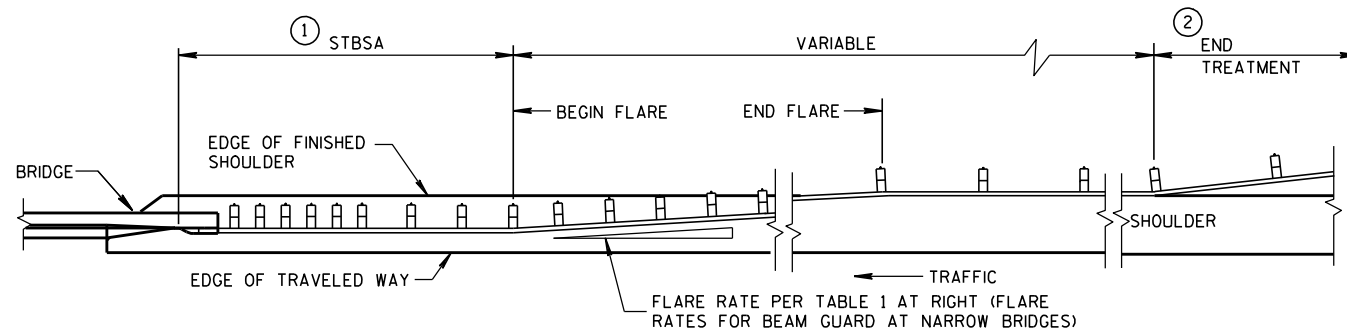


BEAM GUARD AT OBSTACLES - TWO WAY TRAFFIC

(RAIL TO OBSTACLE CLEARANCE 3'-6" TO 4'-6")

TABLE 1
FLARE RATES FOR BEAM
GUARD AT NARROW BRIDGES

POSTED SPEED (MPH)	FLARE RATE
25	13:1
30	15:1
35	16:1
40	18:1
45	21:1
50	24:1
55	26:1
65	30:1

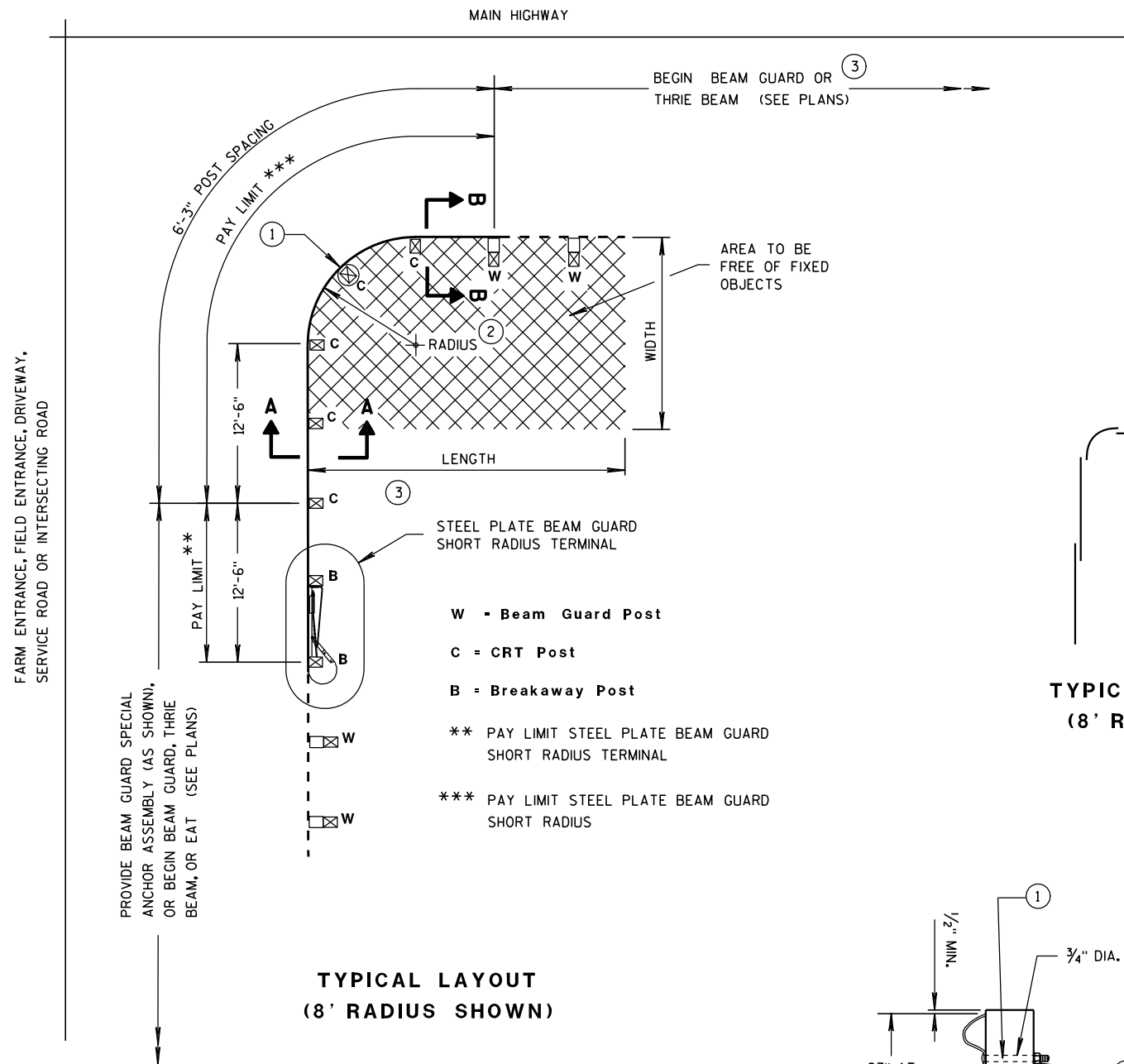


BEAM GUARD AT NARROW BRIDGES (FLARED TO SHOULDER EDGE, THEN PARALLEL TO ROADWAY)

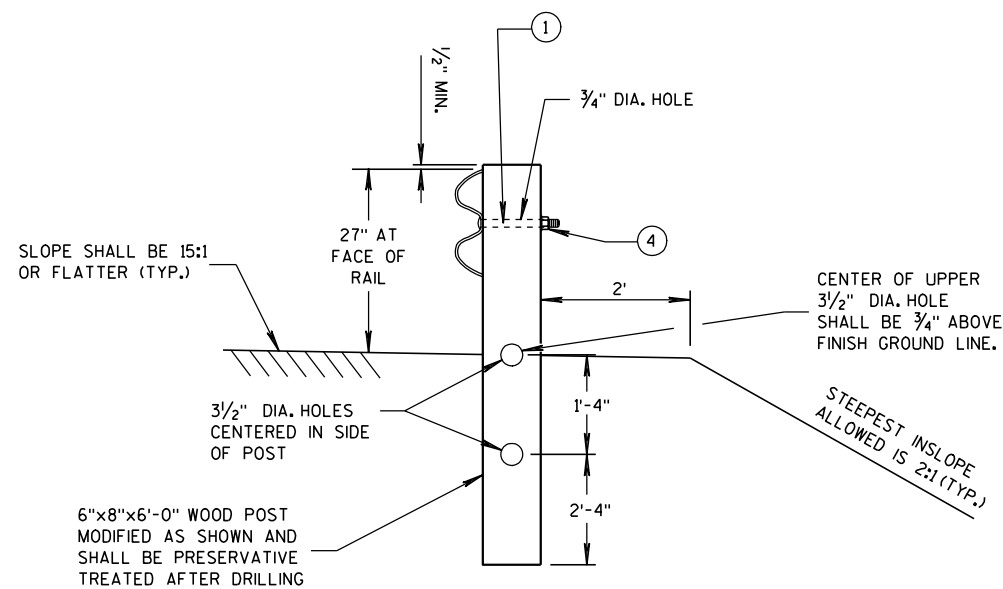
STEEL PLATE BEAM GUARD
CLASS "A"
AT BRIDGES, OBSTACLES
AND SIDEROADS/DRIVEWAYS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
8-21-07
DATE
/S/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT
ENGINEER
FHWA



TYPICAL LAYOUT
(8' RADIUS SHOWN)



SECTION A-A
(CRT POST)

TYPICAL LAP SPLICES
(8' RADIUS SHOWN)

GENERAL NOTES

ALL ANGLES, CHANNELS, AND PLATES SHALL CONFORM TO THE REQUIREMENTS OF ASTM A36 AND THE STRUCTURAL TUBING SHALL CONFORM TO ASTM A 500. WELDING SHALL MEET THE CURRENT REQUIREMENTS OF THE AMERICAN WELDING SOCIETY STRUCTURAL WELDING CODE ANSI/AWS D1.1. ALL STRUCTURAL STEEL SHALL BE GALVANIZED IN ACCORDANCE WITH ASTM A 123. PUNCHING, DRILLING, CUTTING, OR WELDING WILL NOT BE PERMITTED AFTER GALVANIZING. FURNISH AND INSTALL HARDWARE PER STANDARD SPECIFICATION 614.2, UNLESS NOTED OTHERWISE.

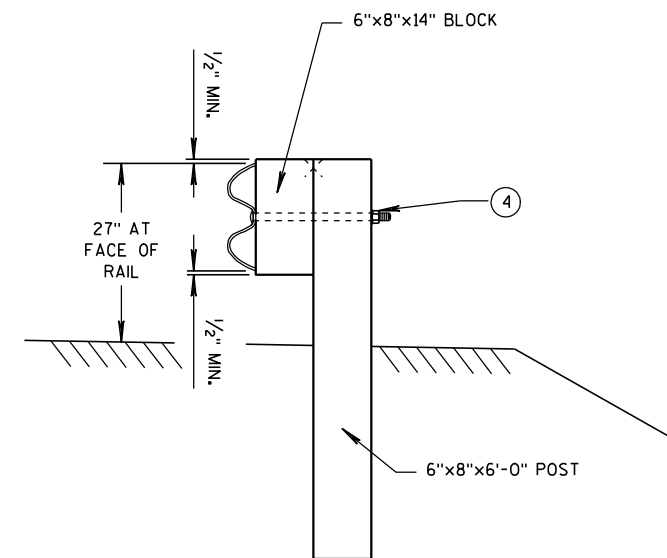
SHOP BEND CURVED RAIL SECTIONS.

SEE STANDARD DETAIL DRAWING 14 B 15 FOR OTHER DETAIL.

- 1 ON THE 8 FOOT RADIUS INSTALLATION, DO NOT INSTALL BUTTON HEAD BOLT AT CENTER CRT POST.
- 2 RADIUS FROM 8' - 36'. SEE PLAN.
- 3 HEIGHT TRANSITION MAY BE REQUIRED. SEE PLAN OR PROJECT ENGINEER.
- 4 5/8" Ø X 1'-6" BUTTON HEAD BOLT AND RECESS NUT WITH ROUND WASHER UNDER NUT.

RADIUS	NUMBER OF CRT POSTS	*NUMBER AND LENGTH OF CURVED RAILS	REQUIRED AREA FREE OF FIXED OBJECTS (LENGTH x WIDTH)
8'	5	1 at 12.5'	25' x 15'
16'	7	1 at 25'	30' x 15'
24'	9	1 at 25' and 1 at 12.5'	40' x 20'
32'	11	2 at 25'	50' x 20'

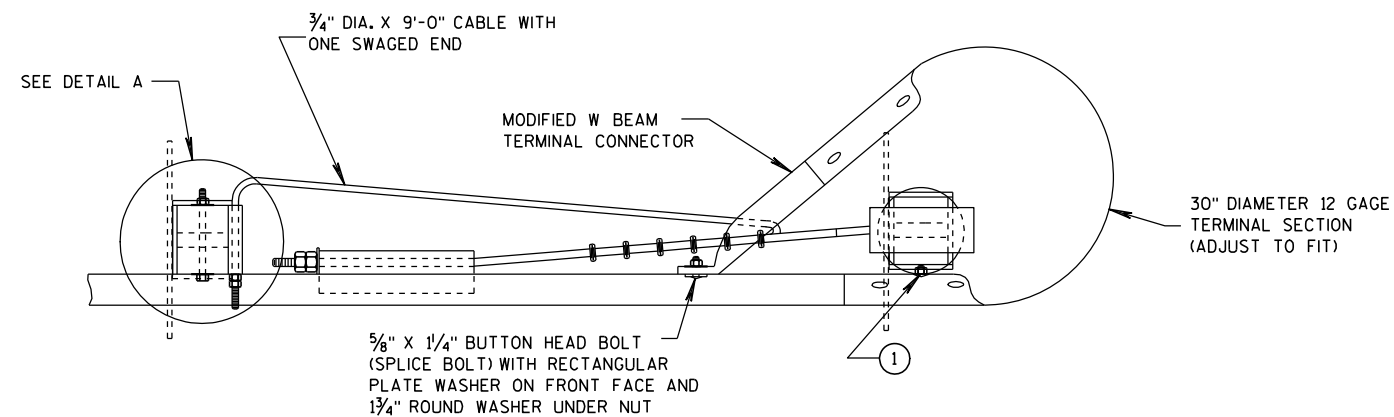
* THE NUMBER OF RAILS IS BASED ON A 90° INTERSECTION. SEE PLAN FOR NON 90° INSTALLATIONS.



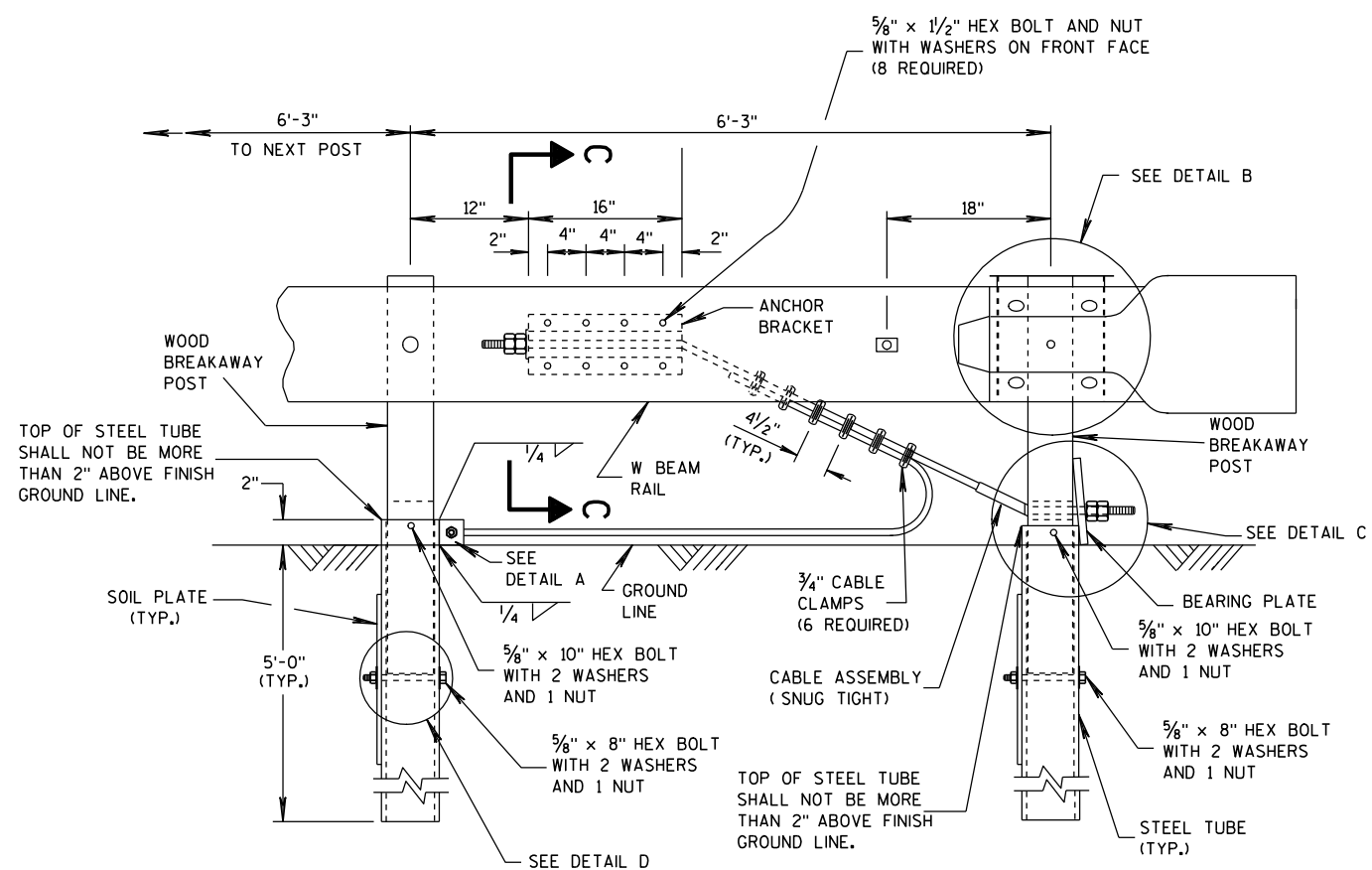
SECTION B-B
(BEAM GUARD POST)

STEEL PLATE BEAM GUARD
SHORT RADIUS TERMINAL

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



PLAN VIEW

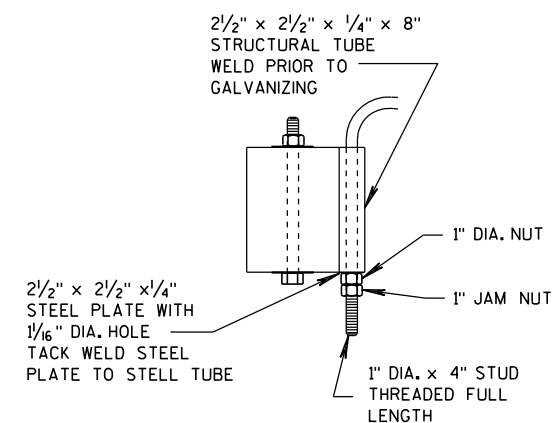


ELEVATION VIEW

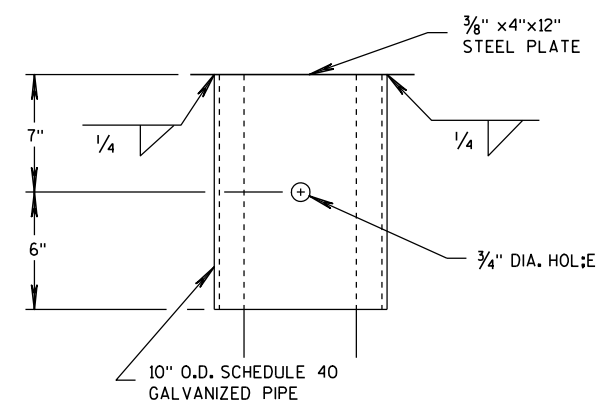
STEEL PLATE BEAM GUARD SHORT RADIUS TERMINAL

GENERAL NOTES

- 1 ATTACH W BEAM RAIL TO THE STEEL PIPE WITH A 5/8" X 2" BUTTON HEAD BOLT WITH NO WASHER. CONNECTION TO THE POST IS NOT REQUIRED.
- INSTALL GALVANIZED 3/4" (6X19) PREFORMED WIRE OR INDEPENDENT WIRE ROPE CORE CONFORMING TO AASHTO M 30. MANUFACTURE WIRE ROPE OUT OF IMPROVED PLOW STEEL WITH A MINIMUM BREAKING STRENGTH OF 42,800 PSI.



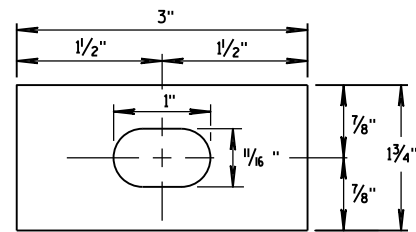
DETAIL A



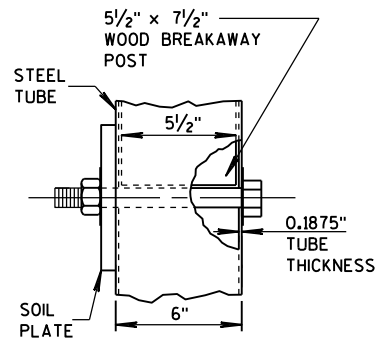
DETAIL B

(BEAM GUARD AND TERMINAL SECTION NOT SHOWN)

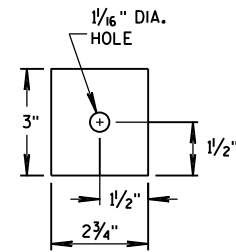
STEEL PLATE BEAM GUARD
SHORT RADIUS TERMINALSTATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



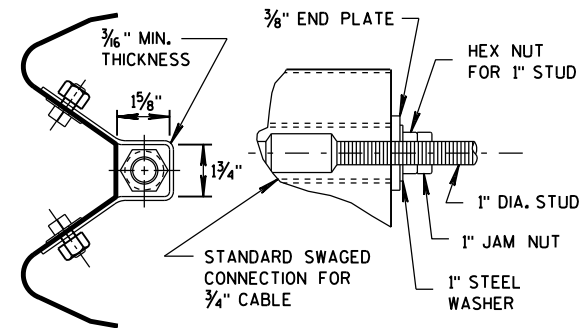
**RECTANGULAR
PLATE WASHER**



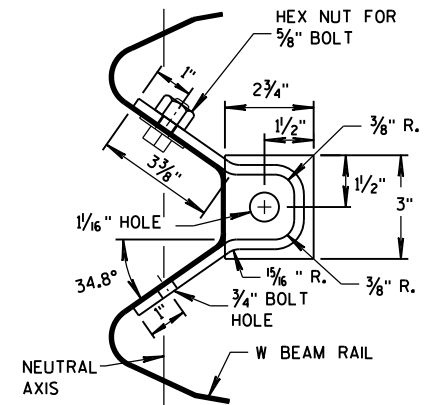
DETAIL D



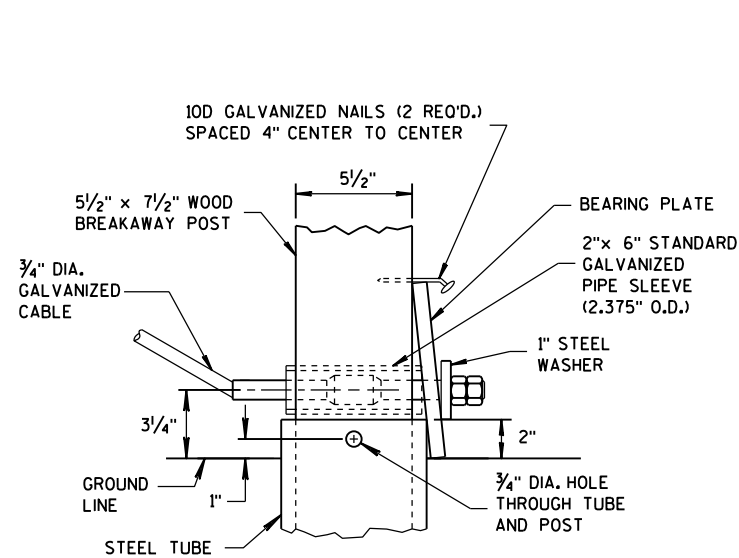
END PLATE



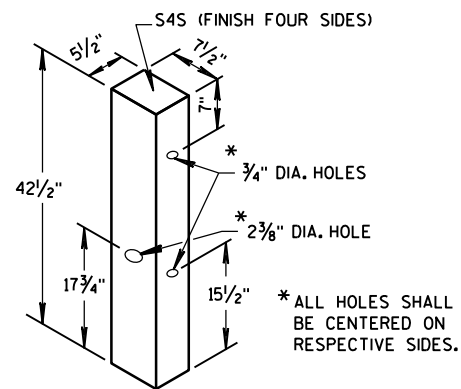
**SECTION C-C
(END PLATE REMOVED)**



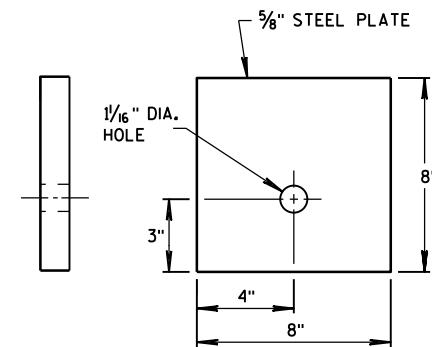
ANCHOR BRACKET



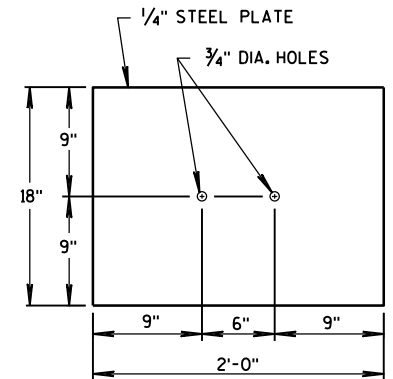
DETAIL C



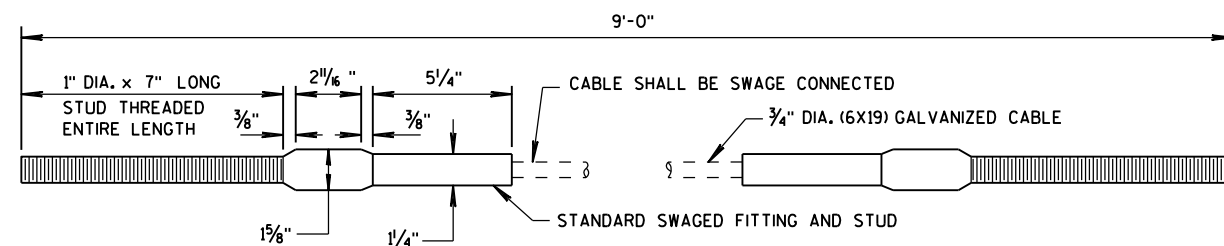
WOOD BREAKAWAY POST



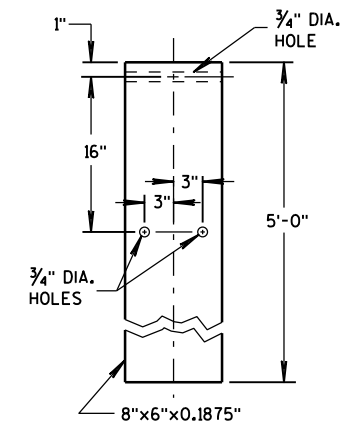
BEARING PLATE



SOIL PLATE



CABLE ASSEMBLY



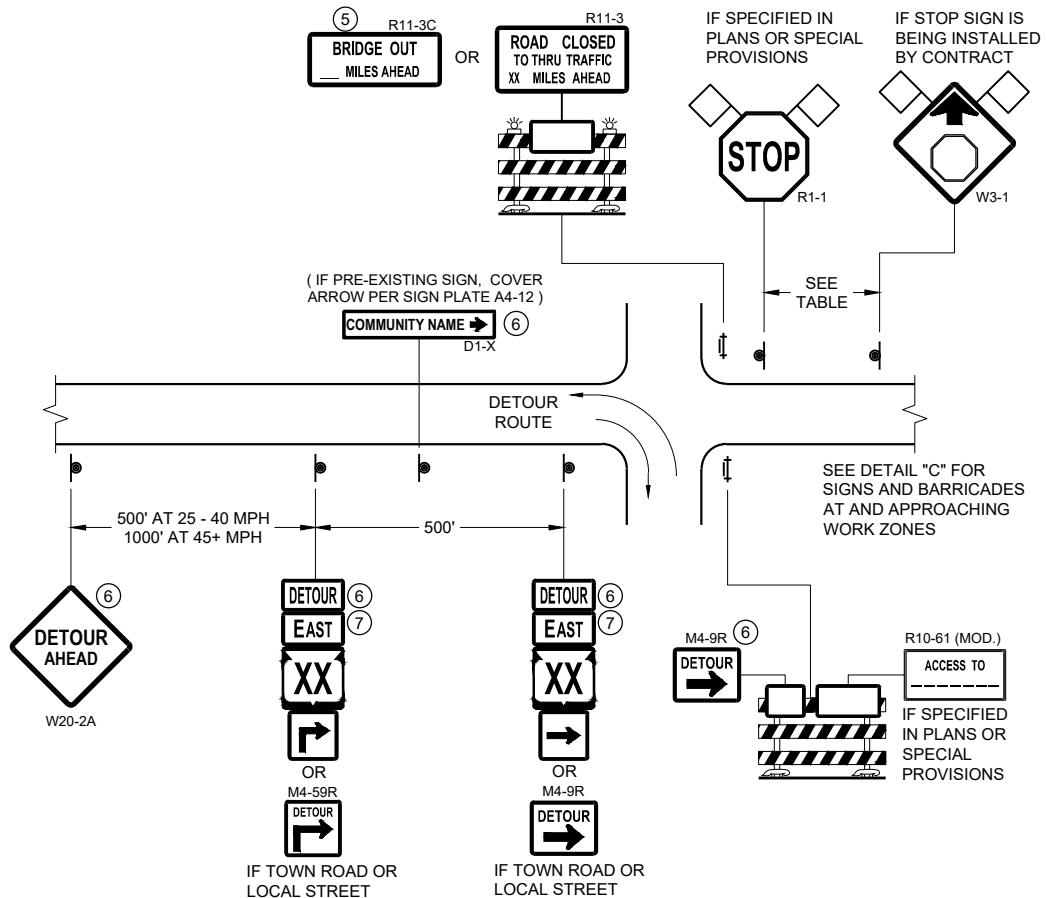
STEEL TUBE

**STEEL PLATE BEAM GUARD
SHORT RADIUS TERMINAL**

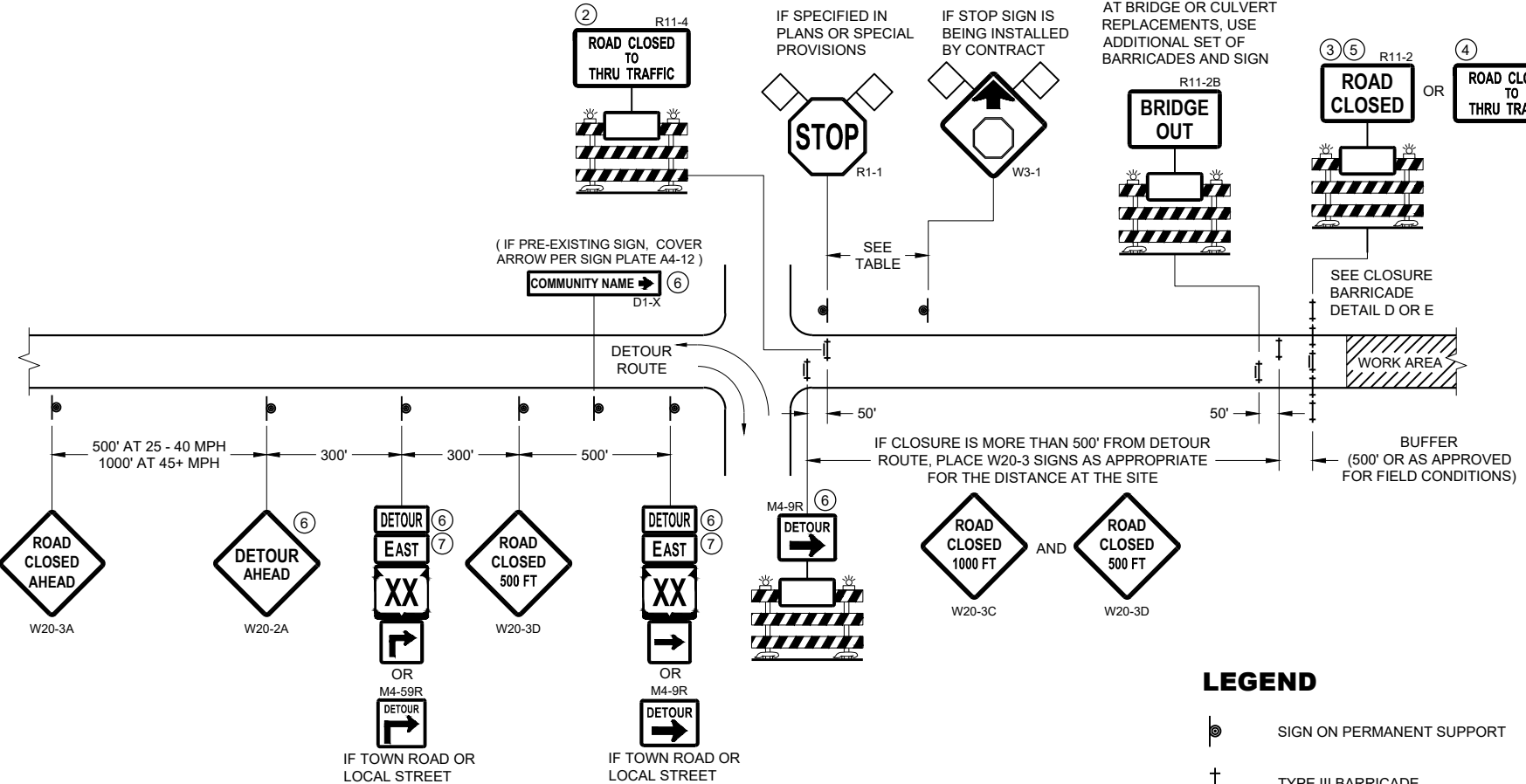
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
12/18/08
DATE
FHWA

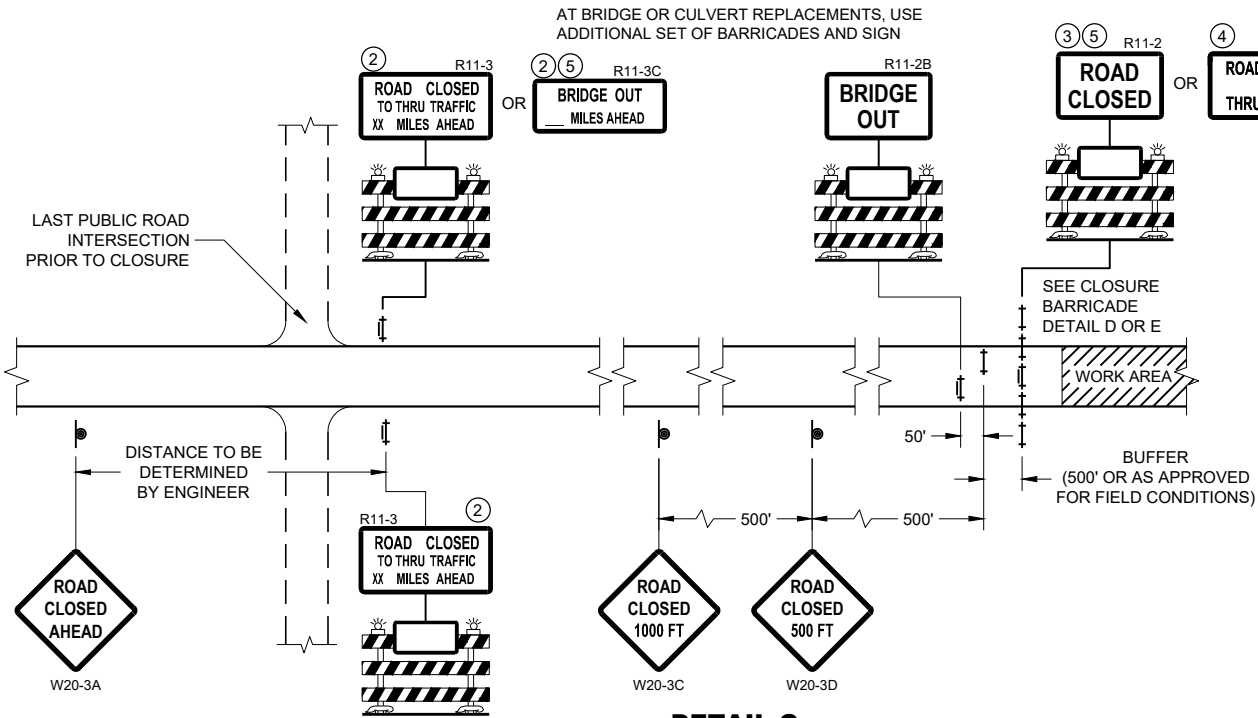
/S/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT
ENGINEER



DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR
WORK ZONE GREATER THAN OR EQUAL TO ½ MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)



DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR
WORK ZONE LESS THAN ½ MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)



DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

SEE SDD 15C2-SHEET "b"
FOR GENERAL NOTES
AND FOOTNOTES ① THROUGH ⑦

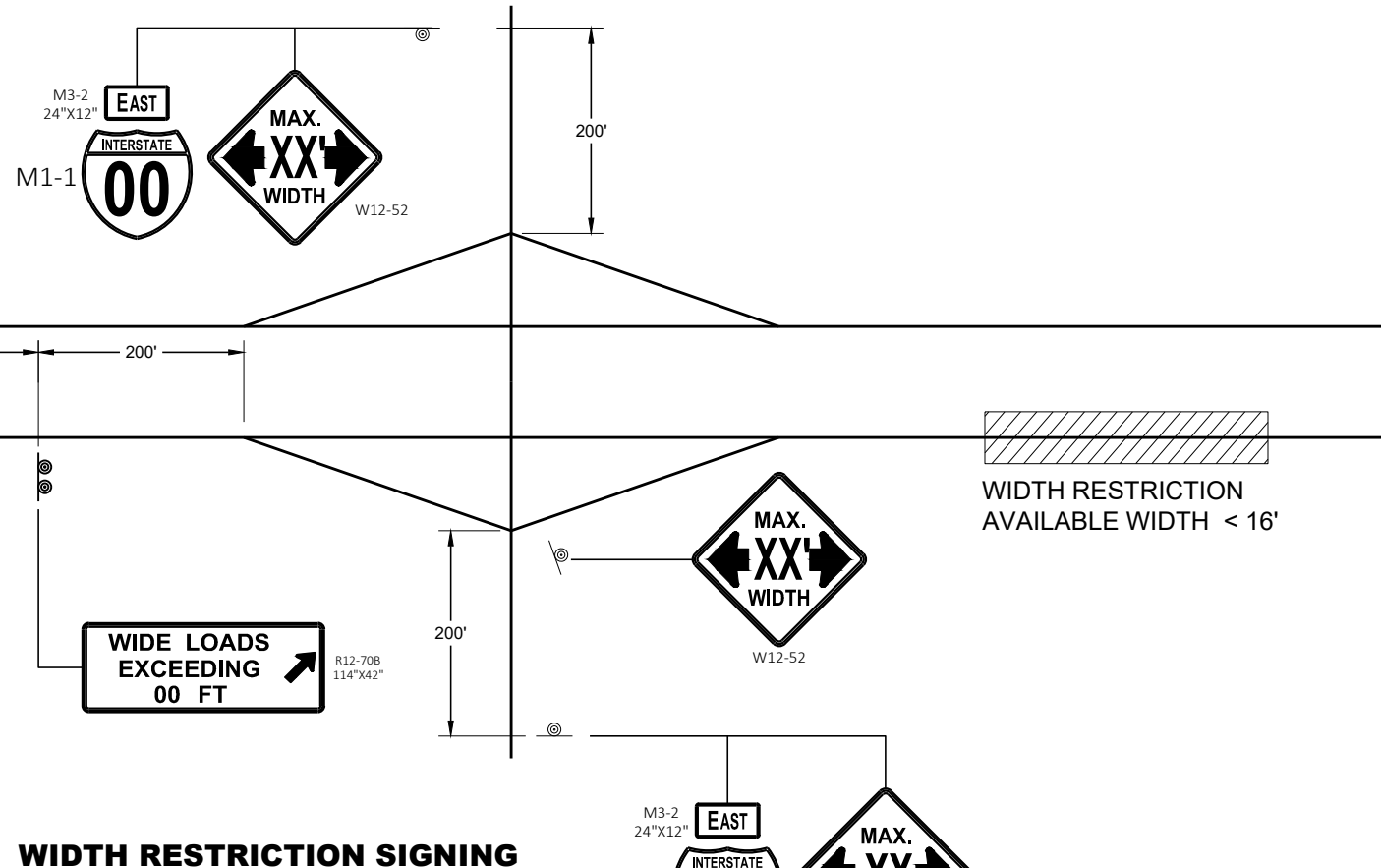
LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
- FLAGS, 16" X 16" MIN. (ORANGE)
- DETOUR M4 - 8
- EAST M3 - X
- XX M1 - 4 OR XX M1 - 6 OR COUNTY M1 - 5A
- OR M05 - 1 OR M06 - 1

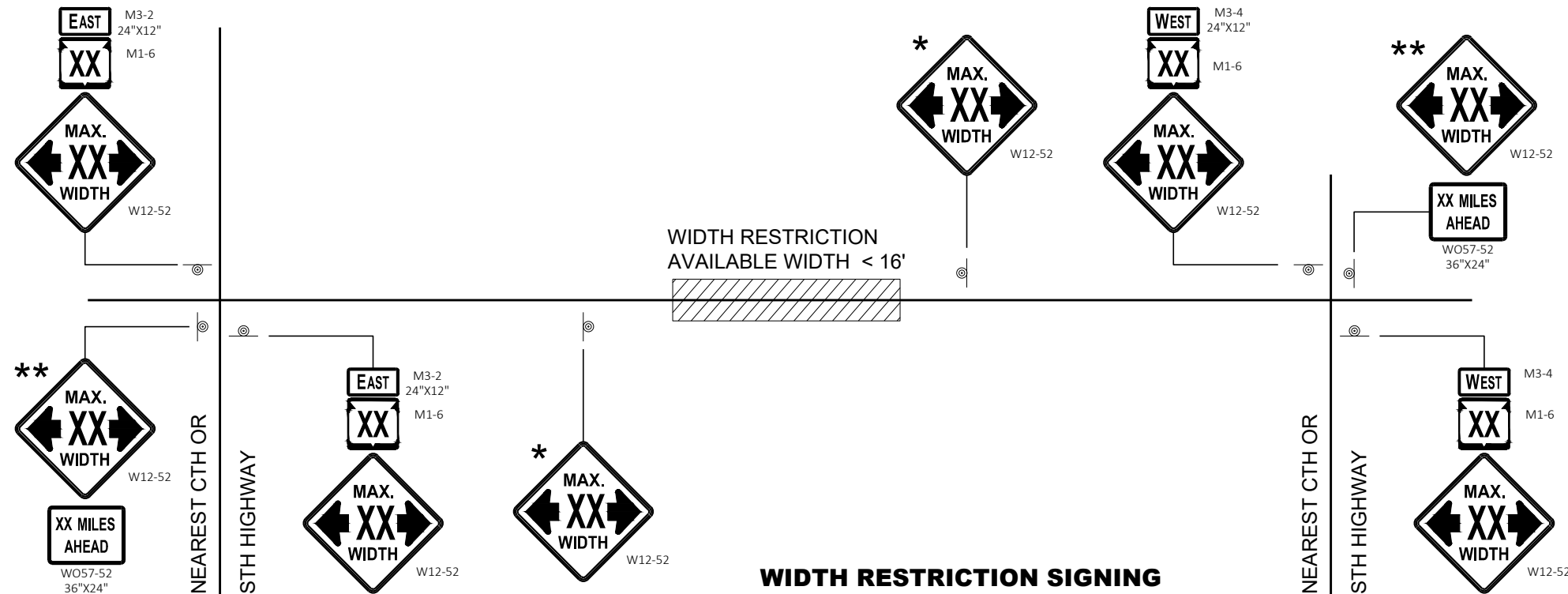
**BARRICADES AND SIGNS
FOR MAINLINE CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER
FHWA



WIDTH RESTRICTION SIGNING



WIDTH RESTRICTION SIGNING
2 LANE HIGHWAY

LEGEND

⊙ SIGN ON PERMANENT SUPPORT

GENERAL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

WIDTH ON SIGN TO BE APPROXIMATELY ONE FOOT LESS THAN AVAILABLE WIDTH.

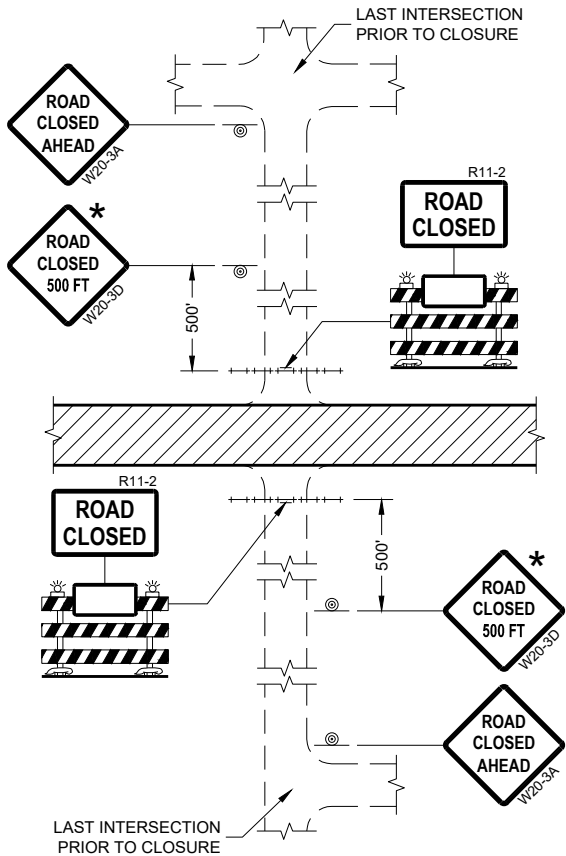
- * PLACE 500 FEET AFTER THE W20 - 1A AND 500 FEET BEFORE ADDITIONAL SIGNS FOR ROADWAYS WITH A PRE - CONSTRUCTION SPEED LIMIT OF 45 MPH OR MORE. FOR 35-40 MPH, USE 350 FOOT TYPICAL SPACING. FOR 25-30 MPH, USE 200 FOOT TYPICAL SPACING.
- ** SIGN SHALL BE VISIBLE FROM ROADWAY.
- *** ADDITIONAL SIGNS NEEDED IF THERE IS AN ON RAMP BETWEEN SIGNS.



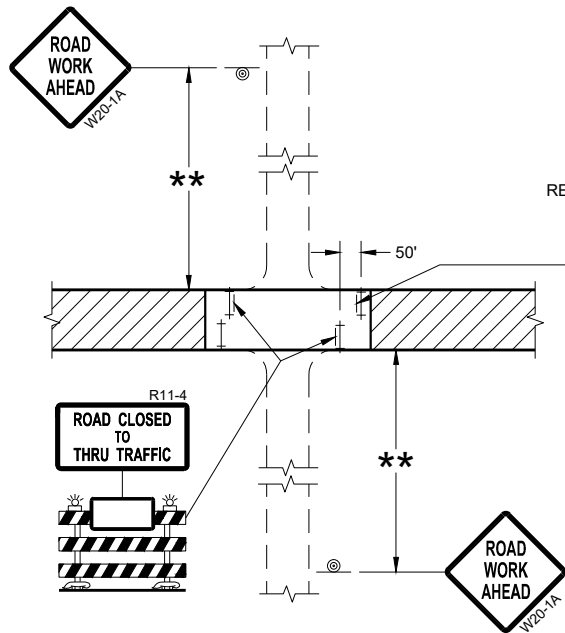
ADVANCED WIDTH
RESTRICTION SIGNING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

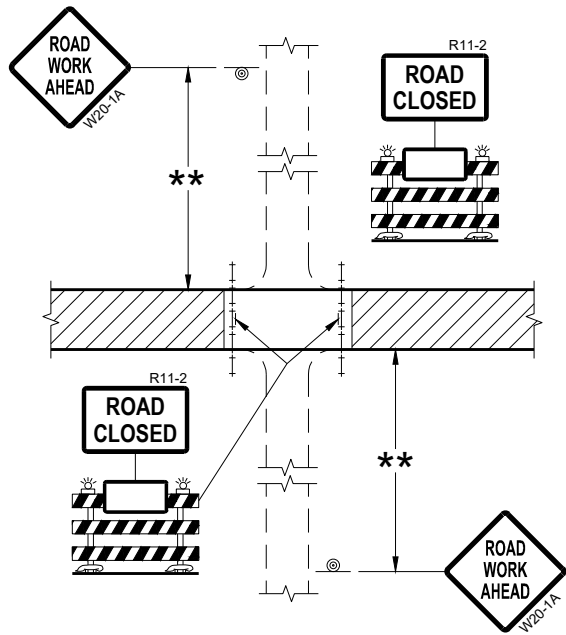
APPROVED
May 2023 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER
FHWA



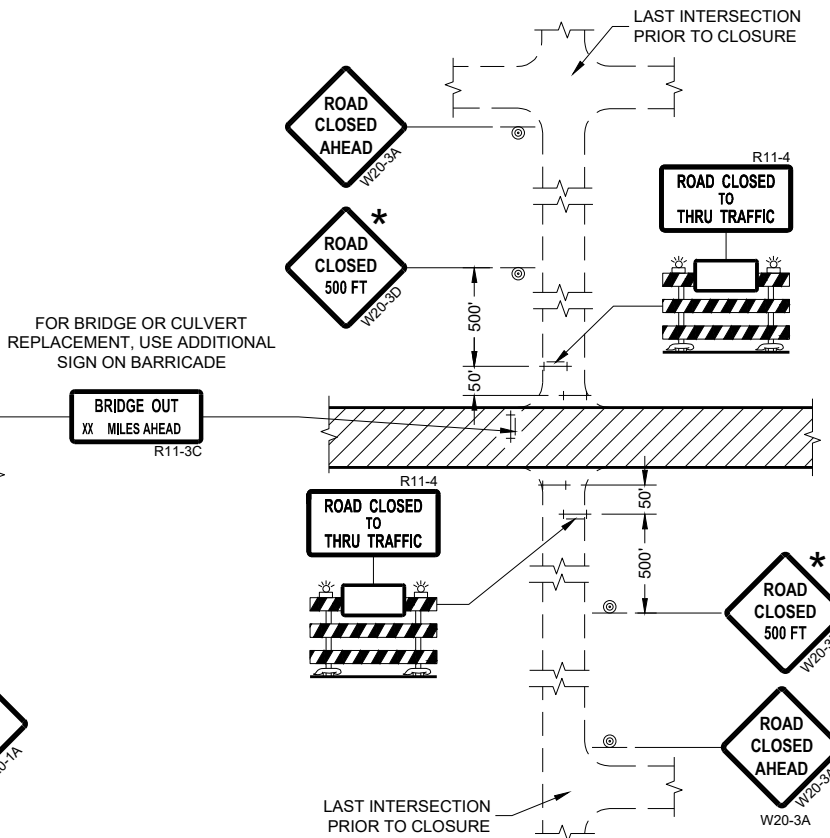
DETAIL 1
(NO ACCESS TO PROJECT)



DETAIL 3
(PUBLIC CROSS-TRAFFIC MAINTAINED.
CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)



DETAIL 2
(PUBLIC CROSS-TRAFFIC MAINTAINED.
NO ACCESS TO PROJECT)



DETAIL 4
(CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE (500 FEET DESIRABLE) TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3, AND R11-4 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
R11-2 SHALL BE 48" X 30".
R11-4 AND R11-3 SHALL BE 60" X 30".

- * OMIT THE "ROAD CLOSED 500 FT." SIGN IF THE LAST INTERSECTION IS 500 FEET OR LESS FROM THE WORK ZONE.
- ** 500' MAX. OR AT LAST INTERSECTION, WHICHEVER IS CLOSEST.

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA

BARRICADES AND SIGNS FOR SIDEROAD CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
July 2018 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.




ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS, 36"X36" SIGNS MAY BE USED INSTEAD OF 48" X 48" SIGNS.

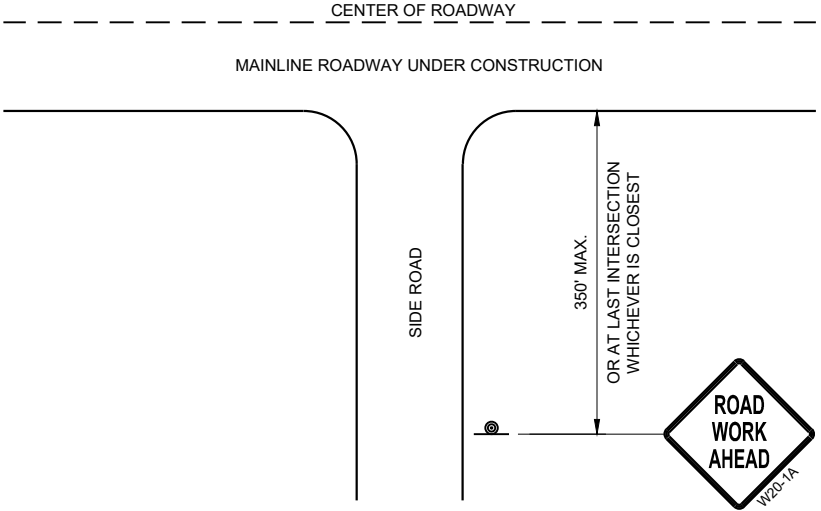
SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

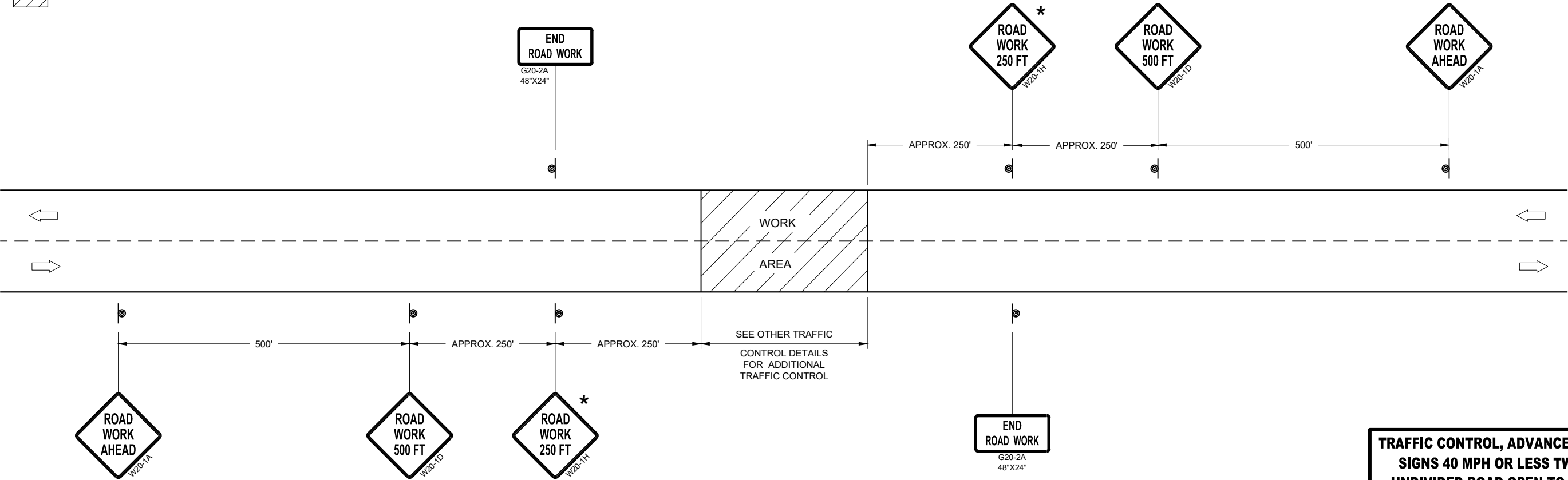
* THE THIRD W20-1 SIGN IS REQUIRED ONLY IF THERE IS AN INTERSECTION BETWEEN THE "ROAD WORK 500 FEET" SIGN AND THE WORK ZONE. ADJUST THE PLACEMENT OF THIS SIGN BASED ON INTERSECTION LOCATION AND OTHER FIELD CONDITIONS.

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC
-  WORK AREA



TYPICAL SIDE ROAD APPROACH
WARNING SIGN DETAIL

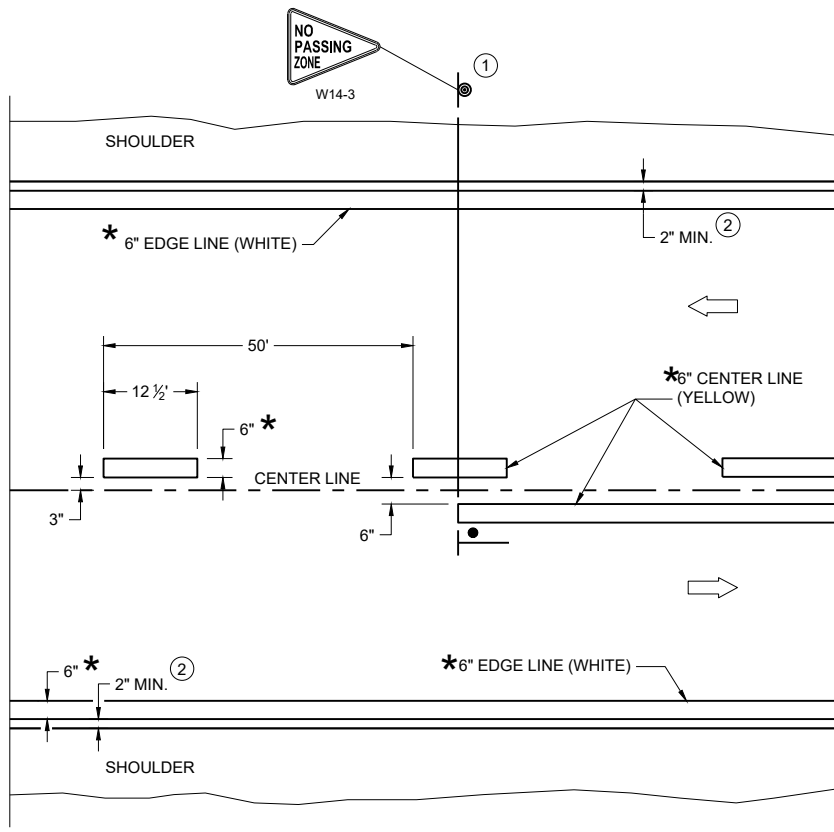


TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40MPH OR LESS

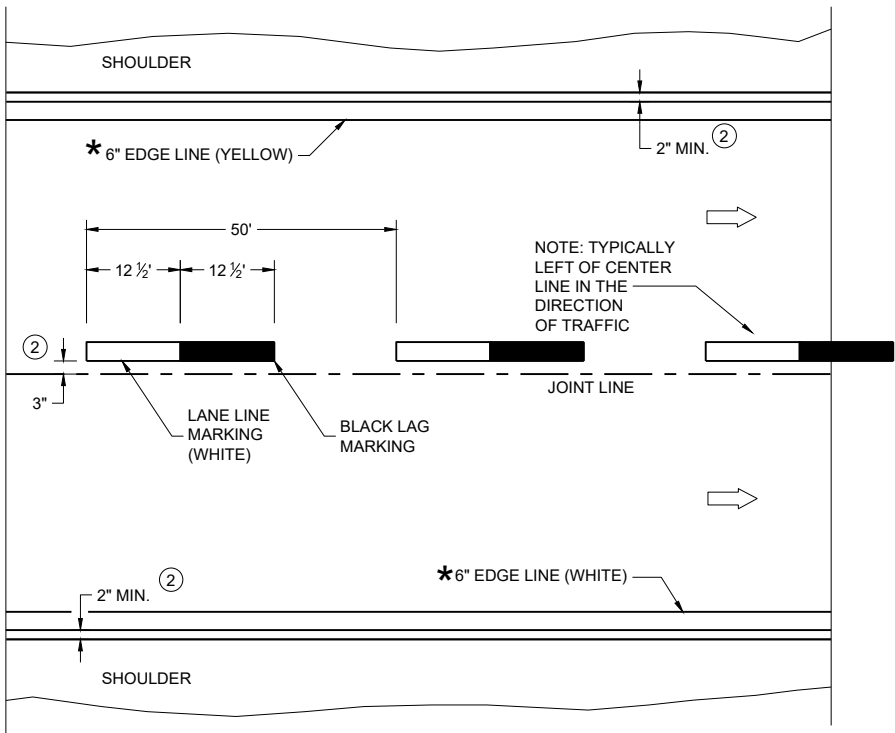
TRAFFIC CONTROL, ADVANCE WARNING
SIGNS 40 MPH OR LESS TWO-WAY
UNDIVIDED ROAD OPEN TO TRAFFIC

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
July 2018 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER
FHWA



TWO WAY TRAFFIC



ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING

*CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES

GENERAL NOTES

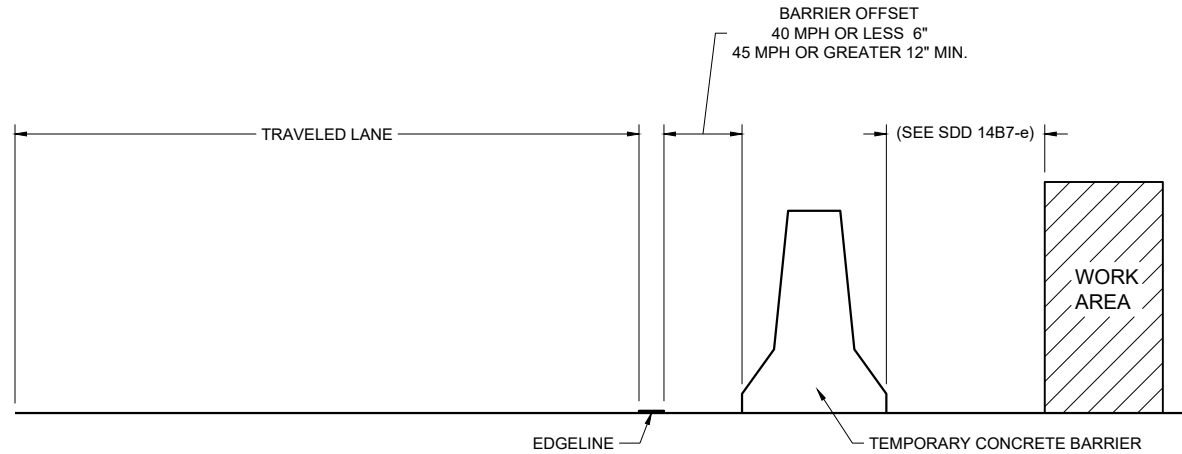
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

- ① LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING
- ② MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

LEGEND

- "T" MARKING
- SIGN ON PERMANENT SUPPORT
- DIRECTION OF TRAFFIC

PERMANENT LONGITUDINAL PAVEMENT MARKINGS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2023 DATE	/S/ Jeannie Silver Statewide Pavement Marking Engineer
FHWA	



TEMPORARY BARRIER OFFSET FROM EDGE LINE

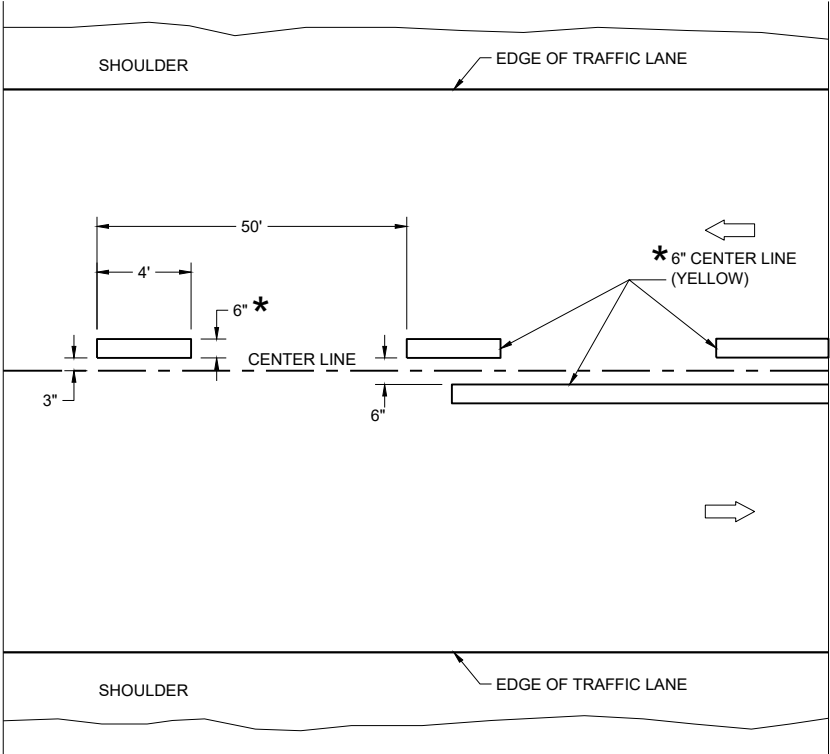
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

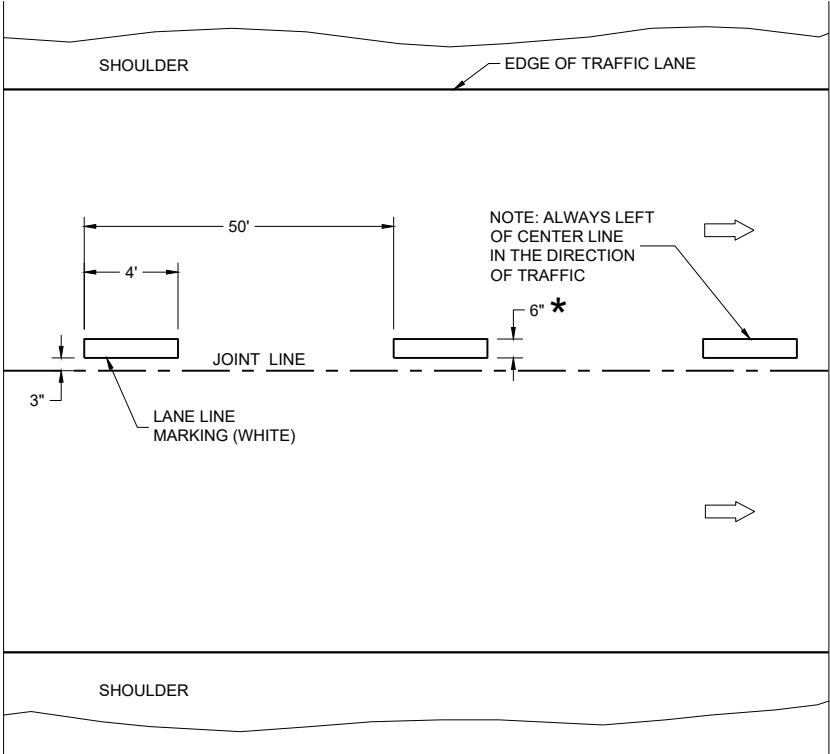
LEGEND

DIRECTION OF TRAFFIC

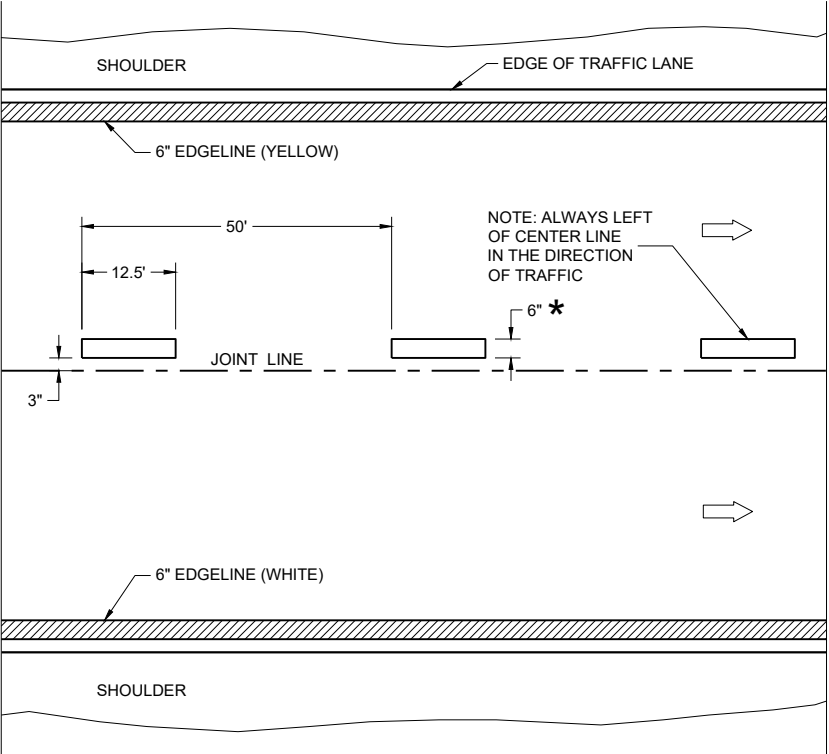
* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES



TWO WAY TRAFFIC



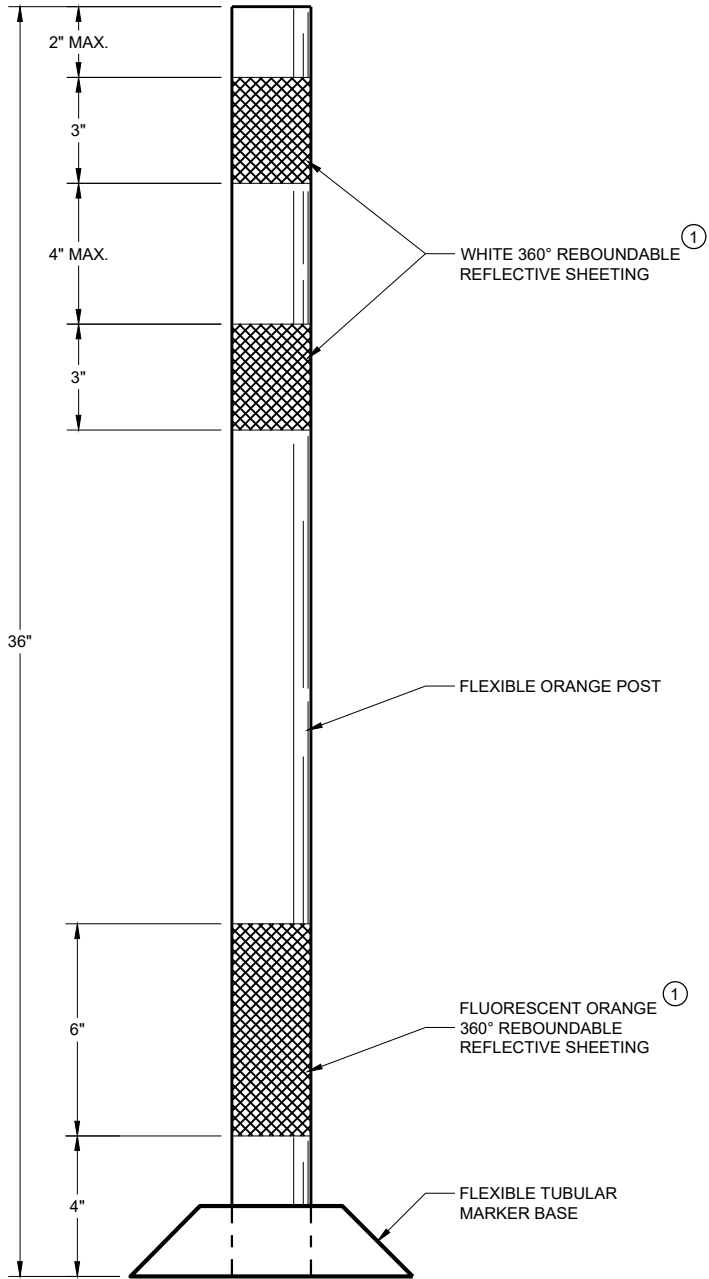
ONE WAY TRAFFIC



FREEWAYS AND EXPRESSWAYS

TEMPORARY PAVEMENT MARKING

TEMPORARY LONGITUDINAL PAVEMENT MARKING	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2023 DATE	/S/ Jeannie Silver Statewide Pavement Marking Engineer
FHWA	



FLEXIBLE TUBULAR
MARKER POST
WORK ZONE

GENERAL NOTES

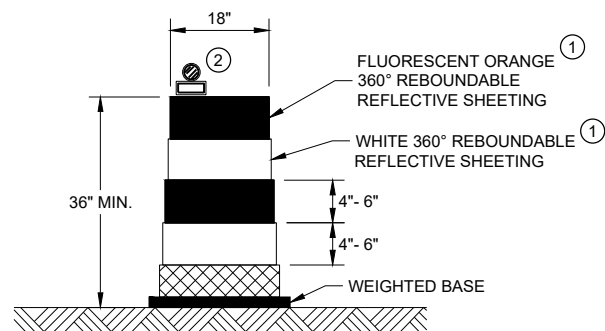
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

SURFACE MOUNTED BASES SHALL BE FURNISHED IN ACCORDANCE WITH MANUFACTURERS RECOMMENDATIONS TO BE COMPATIBLE WITH FLEXIBLE TUBULAR MARKER POSTS TO A SIZE AND SHAPE THAT WILL PROVIDE A STABLE POST FOUNDATION WHEN SECURED TO THE PAVEMENT.

THE ASPHALTIC ADHESIVE OR BUTYL PAD FURNISHED SHALL BE IN ACCORDANCE WITH MANUFACTURERS RECOMMENDATIONS, UNLESS DIRECTED BY THE ENGINEER TO USE BOLTS.

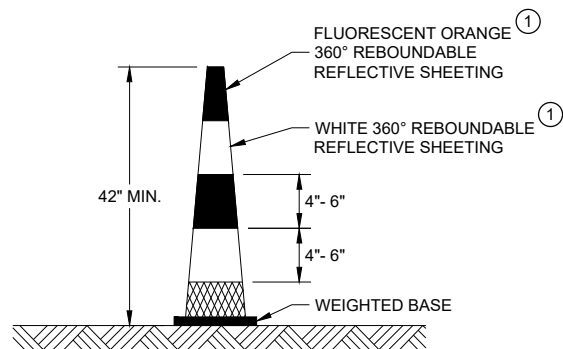
① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.

CHANNELIZING DEVICES FLEXIBLE TUBULAR MARKER POST	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED November 2022 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	



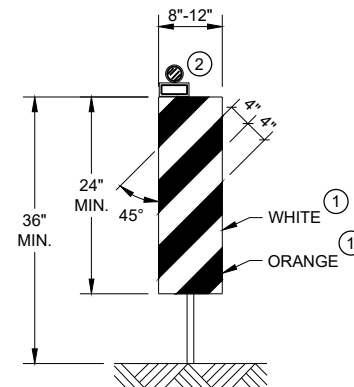
DRUM

BALLAST WIDTHS
RANGE FROM 24"-36"



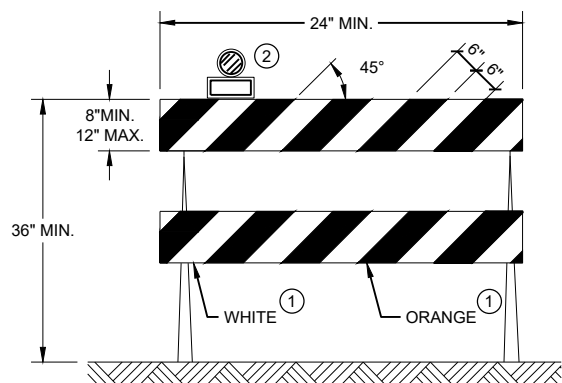
42" CONE

DO NOT USE IN TAPERS
½ SPACING OF DRUMS
BALLAST WIDTHS
RANGE FROM 14"-20"



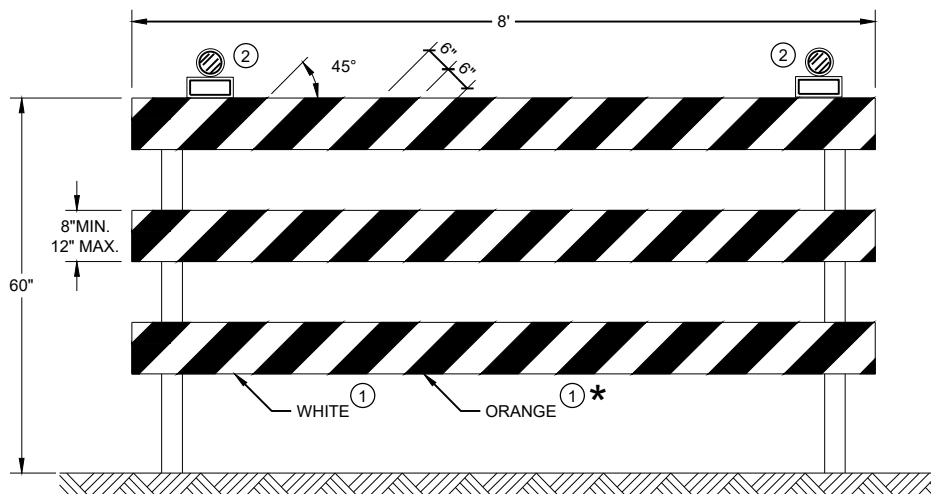
VERTICAL PANEL

THE STRIPES SHALL SLOPE DOWNWARD TO
THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE II BARRICADE

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES
MAY BE USED. ALL STRIPES SHALL SLOPE DOWNWARD
TO THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE III BARRICADE

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP
TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.

* IF USED FOR A PERMANENT APPLICATION USE RED SHEETING.

GENERAL NOTES

- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.






**CHANNELIZING DEVICES
DRUMS, CONES, BARRICADES
AND VERTICAL PANELS**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2022 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA

LEGEND

-  SIGN ON PORTABLE OR PERMANENT SUPPORT
-  TEMPORARY PORTABLE RUMBLE STRIP ARRAY
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS, DEVICES, AND LOCATION OF ALL FLAGGERS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGING

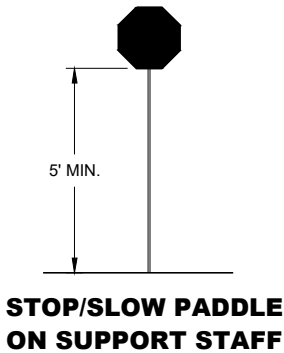
FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT REMOVE TEMPORARY PORTABLE RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.

- ① FOR MOVING WORK OPERATIONS, POST ADDITIONAL W20-7A FLAGGER SIGNS AT APPROXIMATELY 3,500' INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.
- ② SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

WHEN THE DISTANCE BETWEEN FLAGGERS EXCEEDS 2 MILES, A PILOT CAR IS REQUIRED. WHEN CURVES REDUCE SIGHT DISTANCE BELOW 400', A PILOT CAR IS REQUIRED.

TEMPORARY PORTABLE RUMBLE STRIPS

- UTILIZE TEMPORARY PORTABLE RUMBLE STRIPS ON ALL FLAGGING OPERATIONS.
- ③ EACH TEMPORARY PORTABLE RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS PLACED TRANSVERSE ACROSS THE LANE AT THE LOCATIONS SHOWN. WITHIN EACH ARRAY, SPACING BETWEEN RUMBLE STRIPS SHALL BE 15 FEET ON CENTER
- ONLY USE TEMPORARY PORTABLE RUMBLE STRIPS FROM THE APPROVED PRODUCTS LIST.
- INSTALL TEMPORARY RUMBLE STRIPS PER MANUFACTURER'S RECOMMENDATIONS.
- PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.
- DO NOT INSTALL TEMPORARY PORTABLE RUMBLE STRIPS ON GRAVEL, MILLED SURFACES, OR ASPHALT THAT HAS BEEN PAVED LESS THAN 12 HOURS.

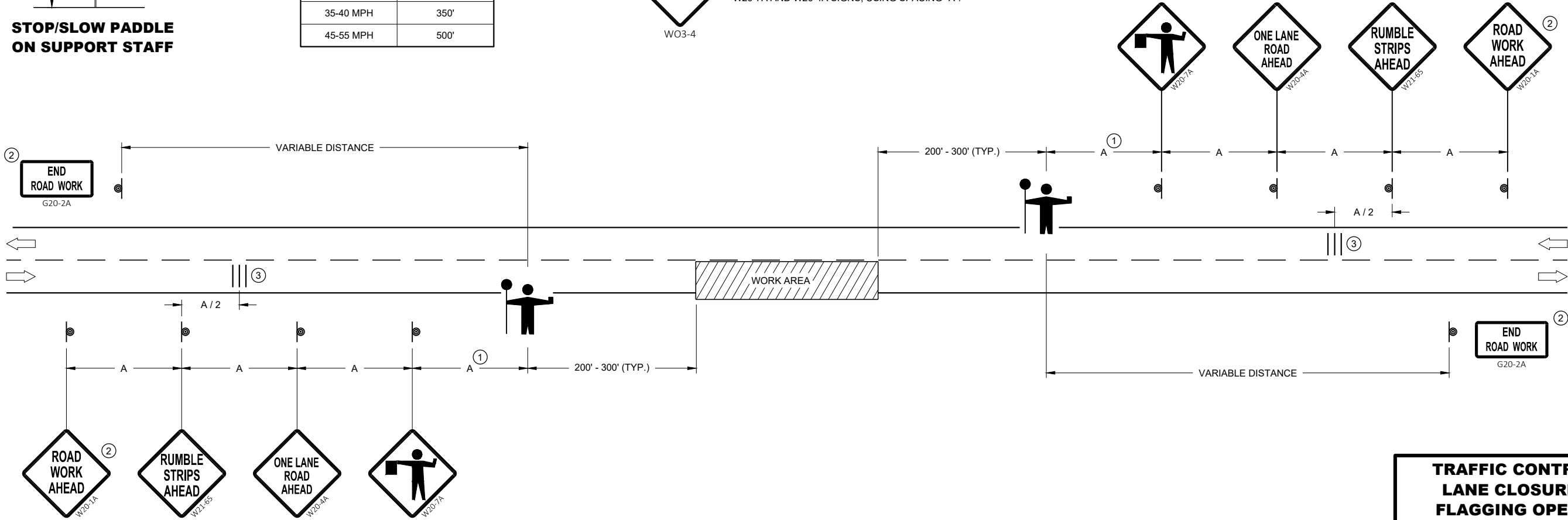


SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE

SPEED LIMIT	SPACING "A"
25-30 MPH	200'
35-40 MPH	350'
45-55 MPH	500'










USE OF W03-4 SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7A AND W20-4A SIGNS, USING SPACING "A".



TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2022 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

GENERAL NOTES

-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL CONE 42-INCH
-  TRAFFIC CONTROL DRUM
-  TEMPORARY PORTABLE RUMBLE STRIP ARRAY
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  AUTOMATED FLAGGER ASSISTANCE DEVICE (AFAD)

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS, DEVICES, AND LOCATION OF ALL FLAGGERS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGING

IF THE AUTOMATED FLAGGER ASSISTANCE DEVICE (AFAD) STOPS WORKING, FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT REMOVE TEMPORARY PORTABLE RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.

WHEN THE DISTANCE BETWEEN FLAGGERS EXCEEDS 2 MILES, A PILOT CAR IS REQUIRED. WHEN CURVES REDUCE SIGHT DISTANCE BELOW 400', A PILOT CAR IS REQUIRED.

- ① SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.
- ② IF FLAGGERS ARE PHYSICALLY NEEDED TO FLAG, REPLACE WO3-4 SIGNS WITH W20-7A SIGNS.

TEMPORARY PORTABLE RUMBLE STRIPS

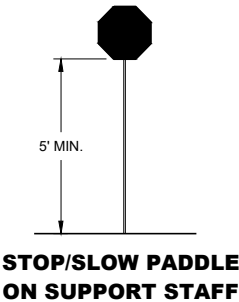
UTILIZE TEMPORARY PORTABLE RUMBLE STRIPS ON ALL FLAGGING OPERATIONS.

ONLY USE TEMPORARY PORTABLE RUMBLE STRIPS FROM THE APPROVED PRODUCTS LIST.

PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.

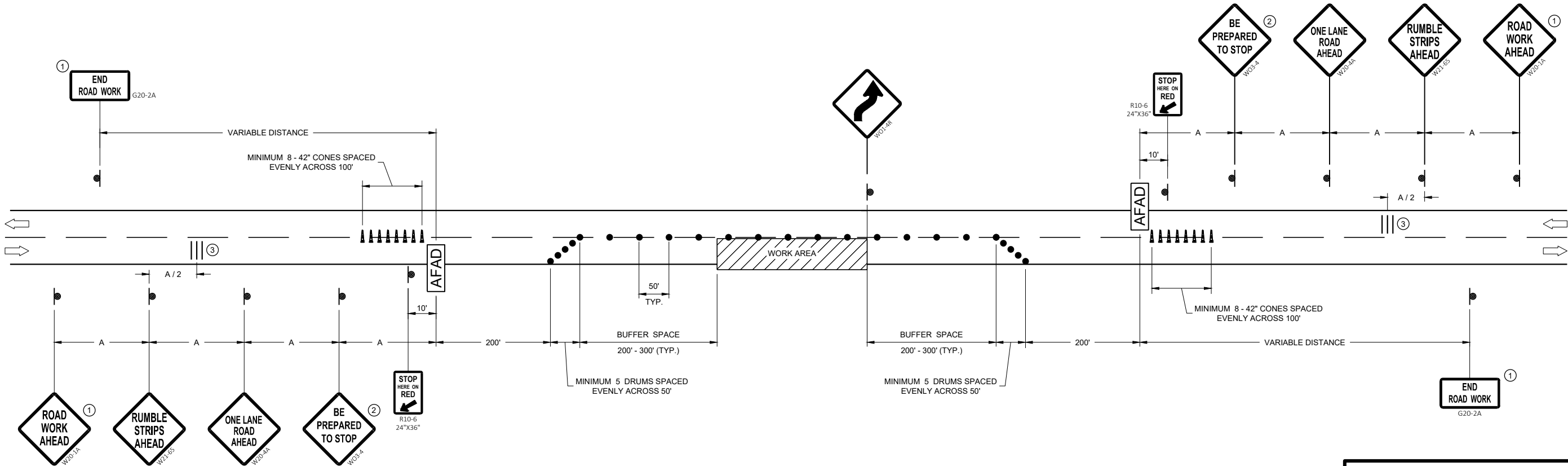
DO NOT INSTALL TEMPORARY PORTABLE RUMBLE STRIPS ON GRAVEL, MILLED SURFACES, OR ASPHALT THAT HAS BEEN PAVED LESS THAN 12 HOURS.

③ EACH TEMPORARY PORTABLE RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS PLACED TRANSVERSELY AT THE LOCATIONS SHOWN. WITHIN EACH ARRAY, SPACING BETWEEN RUMBLE STRIPS SHALL BE 15 FEET ON CENTER.



SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE

SPEED LIMIT	SPACING "A"
25-30 MPH	200'
35-40 MPH	350'
45-55 MPH	500'



TRAFFIC CONTROL, LANE CLOSURE WITH AUTOMATED FLAGGER ASSISTANCE DEVICE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2022 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER


FHWA


LEGEND


- V1

LEAD VEHICLE
- V2

MARKING VEHICLE
- V3

SHADOW VEHICLE
- 

TRUCK MOUNTED ATTENUATOR (TMA)
- 

SIGN ON TEMPORARY SUPPORT
- 

DIRECTION OF TRAFFIC

GENERAL NOTES

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

ALL VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL OPERATING IN CAUTION MODE. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.

DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL AND HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.

THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS.

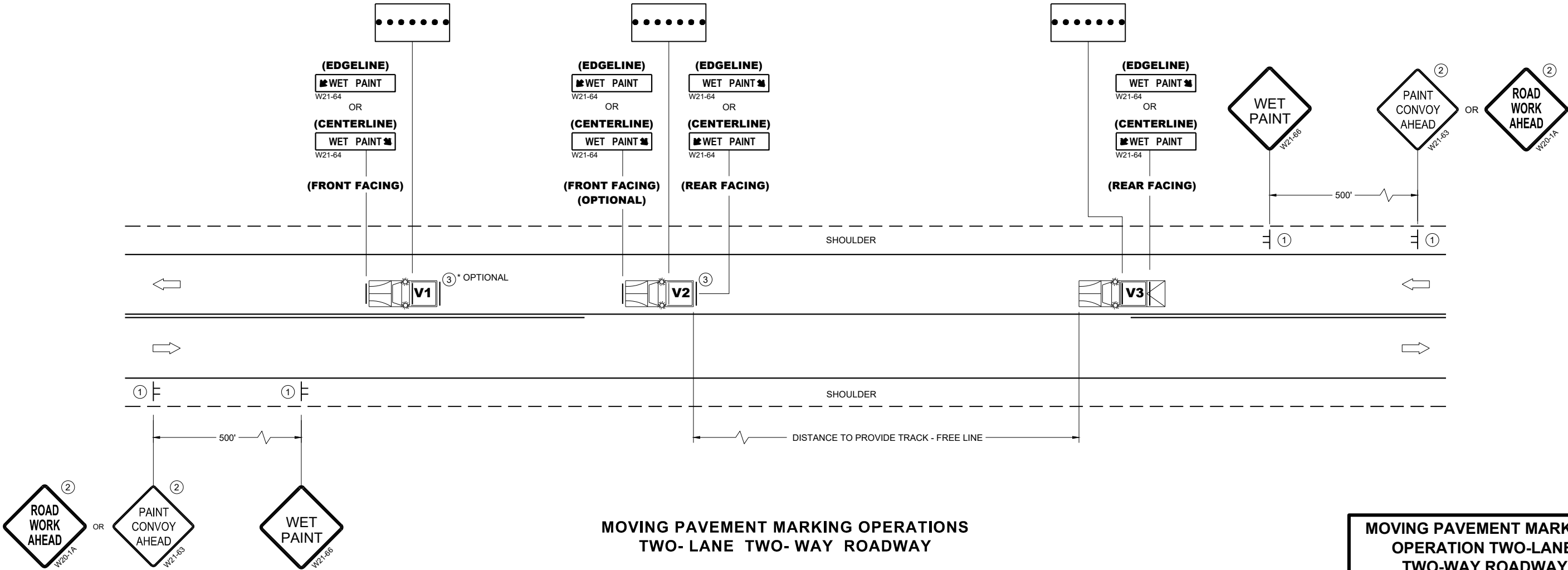
WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR LAY STATIONARY SIGNS AND SUPPORTS FLAT ON THE GRADE WITH

UPRIGHTS ORIENTED PARALLEL TO AND DOWNSTREAM FROM TRAFFIC.

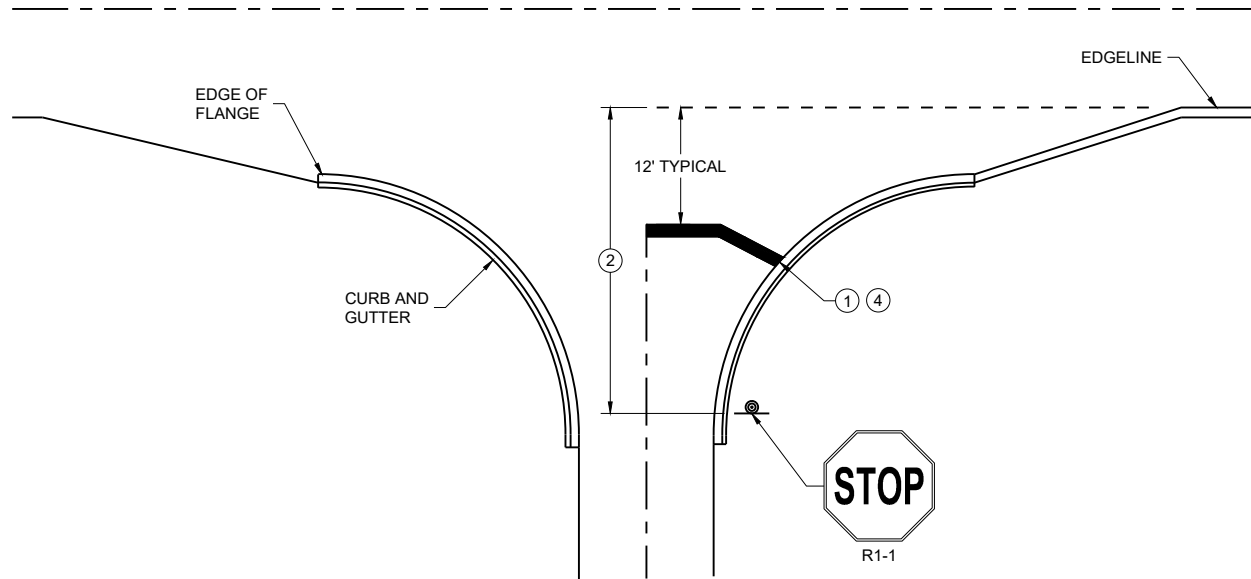
CONES SHOULD BE USED BETWEEN THE MARKING AND SHADOW VEHICLE AT 100 FOOT SPACING. CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.

CONES SHALL BE A MINIMUM OF 28" FOR WET PAVEMENT MARKING .

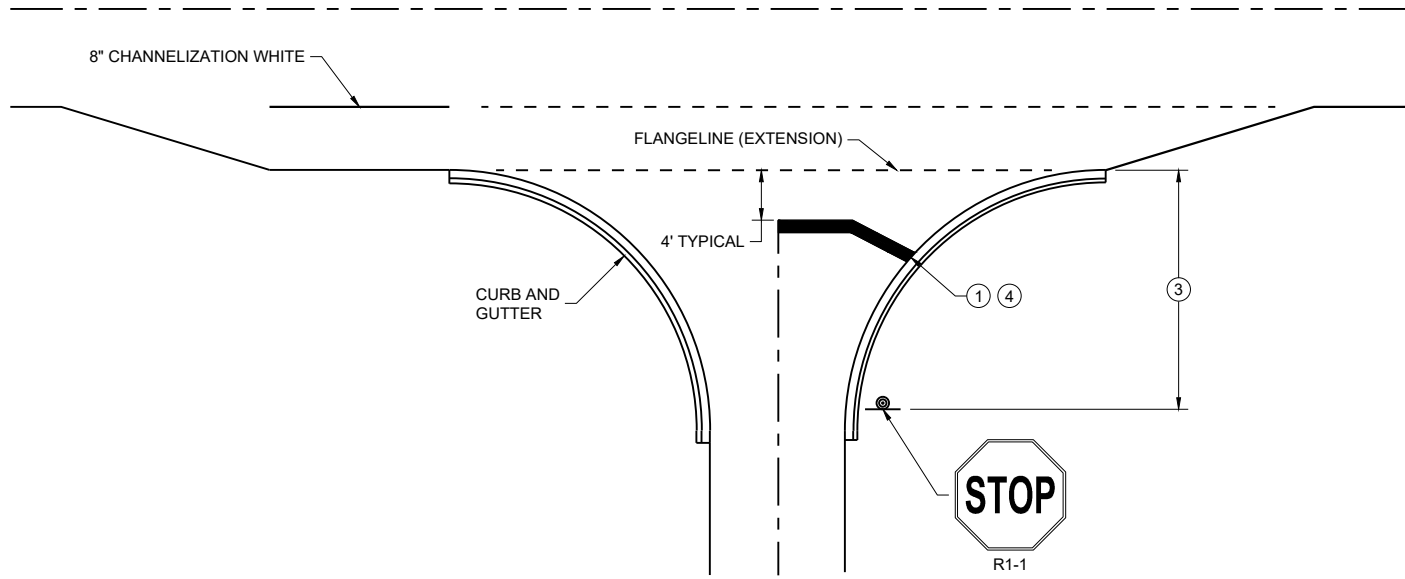
- ① SIGNS SHALL BE REPEATED APPROXIMATELY EVERY THREE MILES AND AFTER EVERY MAJOR INTERSECTION.
- ② IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1A OR W21-63 ARE NOT REQUIRED.
- ③ V1 AND V2 CAN BE SWITCHED SO THAT THE MARKER IS THE LEAD VEHICLE.



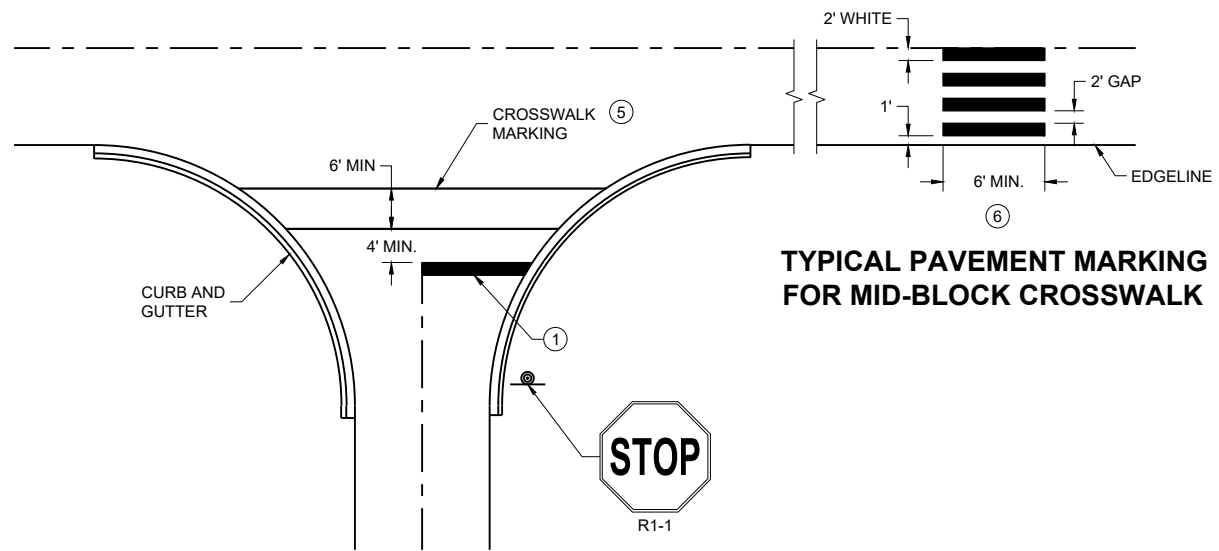
MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED March 2024 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER



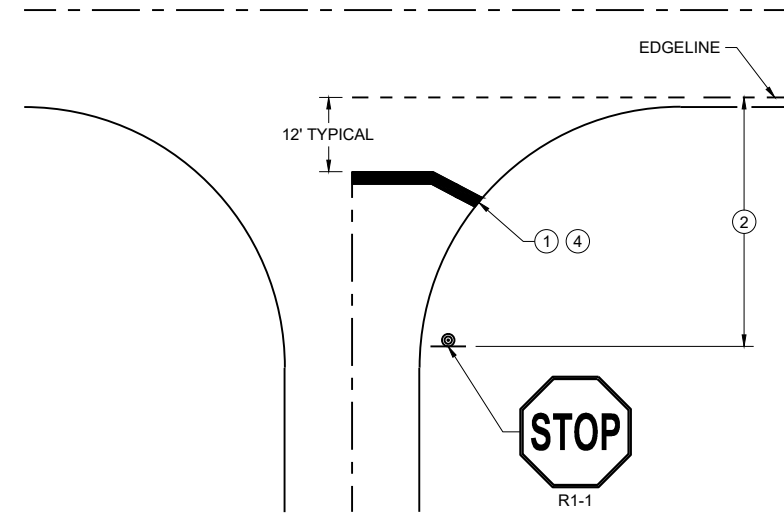
TYPICAL STOP LINE PAVEMENT MARKING
WITH CURB AND GUTTER



TYPICAL STOP LINE PAVEMENT MARKING
FOR SIDE ROADS WITH RIGHT TURN LANE



TYPICAL STOP LINE PAVEMENT MARKING
FOR SIDE ROADS WITH CROSSWALK MARKING



TYPICAL STOP LINE PAVEMENT MARKING
WITHOUT CURB AND GUTTER

GENERAL NOTES

STOP SIGN SHALL BE PLACED A MINIMUM OF 6 FEET TO A MAXIMUM OF 50 FEET FROM THE EDGELINE LOCATION.

- 1 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE REGION MARKING ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
- 2 NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGELINE.
- 3 NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGE LINE EXTENSION.
- 4 MOVE CLOSER TO THE EDGE OF TRAVEL LINE AS NEEDED FOR VISIBILITY AND SIGHT LINES (NO CLOSER THAN 4 FEET).
- 5 LADDER BAR CROSSWALKS SHOULD ONLY BE USED FOR MID BLOCK CROSSINGS. USE 2 - 6" TRANSVERSE LINES.
- 6 POSTED SPEED LIMITS OF 40 MPH OR GREATER USE A MINIMUM WIDTH OF 8' FOR MIDBLOCK CROSSWALKS




STOP LINE AND CROSSWALK
PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
March 2024 DATE /S/ Matthew Rauch
STATE SIGNING AND MARKING
ENGINEER

FHWA

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  DELINEATOR, FLEXIBLE/TUBULAR MARKER
-  DIRECTION OF TRAFFIC

GENERAL NOTES

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

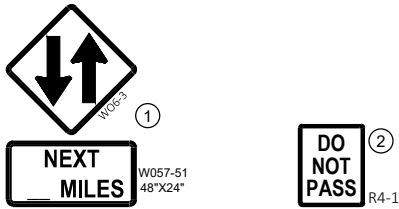
ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER. NO WARNING LIGHTS SHALL BE WORKING ON COVERED OR "DOWNED" SIGNS.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

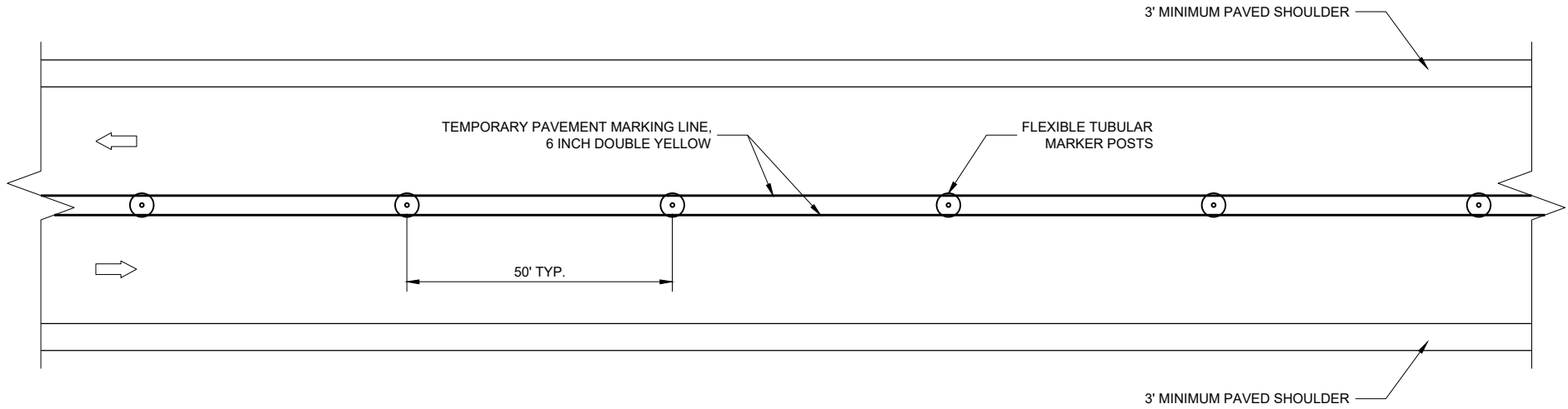
THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500' DESIRABLE) DISTANCE TO EXISTING SIGNS.

A SINGLE ROW OF FLEXIBLE TUBULAR MARKERS ON CENTERLINE EXTEND FOR THE ENTIRE LENGTH OF TWO-WAY TRAFFIC AT 50 FOOT SPACING.

COVER EXISTING CENTERLINE STRIPE WITH TEMPORARY PAVEMENT MARKING LINE, 6 INCH DOUBLE YELLOW.



- ① THE W06-3 AND W057-51 SHALL BE LOCATED 200 FEET BEYOND THE END OF THE ACCELERATION LANE OF EACH ENTRANCE RAMP AND / OR 500 FEET BEYOND ANY SIDE ROAD. THE R4-1 SHALL BE LOCATED 1000 FEET BEYOND THE W06-3 AND THE W057-51 AND THE SIGNS SHALL BE ALTERNATED WITH ONE MILE INTERVALS BETWEEN THE SIGNS.
- ② CONVENTIONAL: 24" X 30"
FREEWAY AND EXPRESSWAY: 36" X 48"



TWO LANE, TWO WAY OPERATION





**TRAFFIC CONTROL
TWO LANE TWO WAY
OPERATION**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED May 2023 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
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FHWA

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  DIRECTION OF TRAFFIC
-  WORK ZONE

GENERAL NOTES

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" SIGNS MAY BE USED IF APPROVED BY THE REGIONAL TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

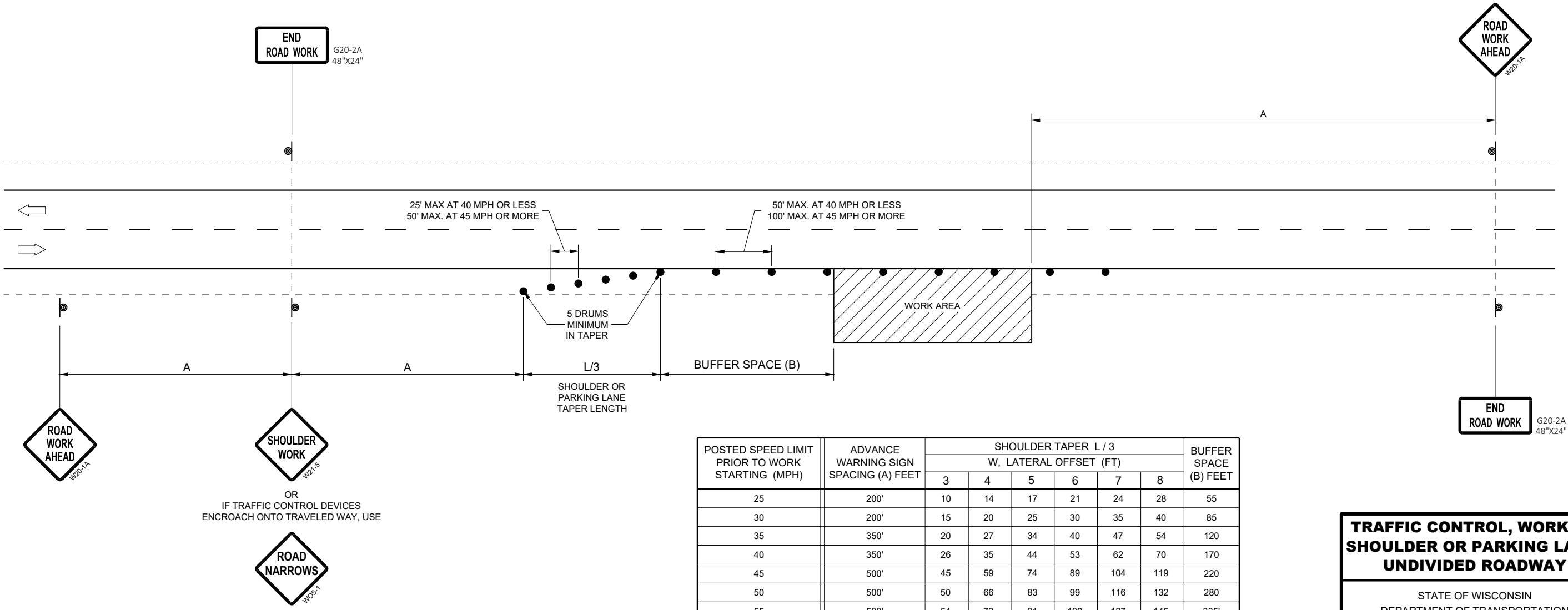
ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

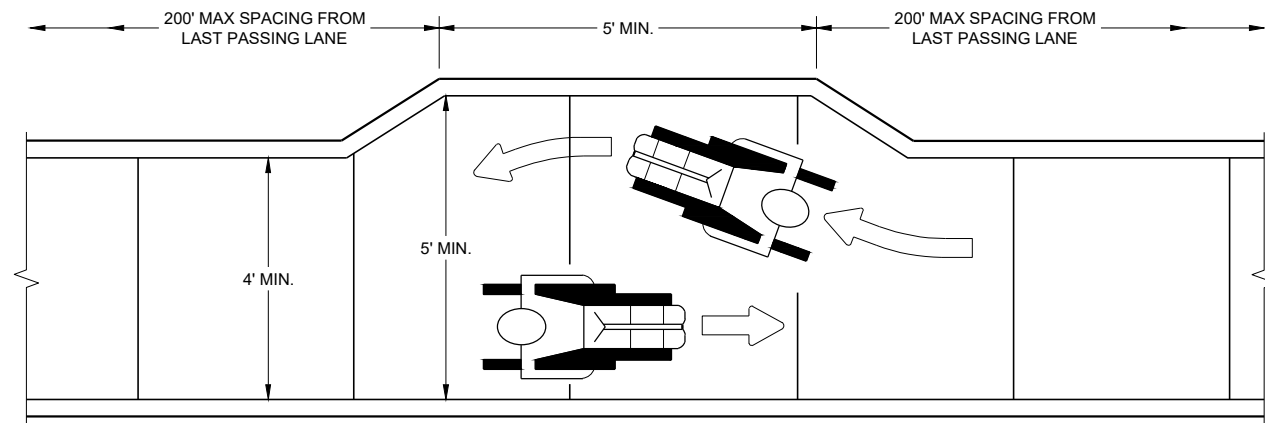
W20-1A AND G20-2A SIGNS ARE NOT REQUIRED IF THE WORK AREA IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT. G20-2A SIGNS MAY ALSO BE OMITTED IF DURATION OF WORK IS LESS THAN 7 CONTINUOUS DAYS AND NIGHTS.



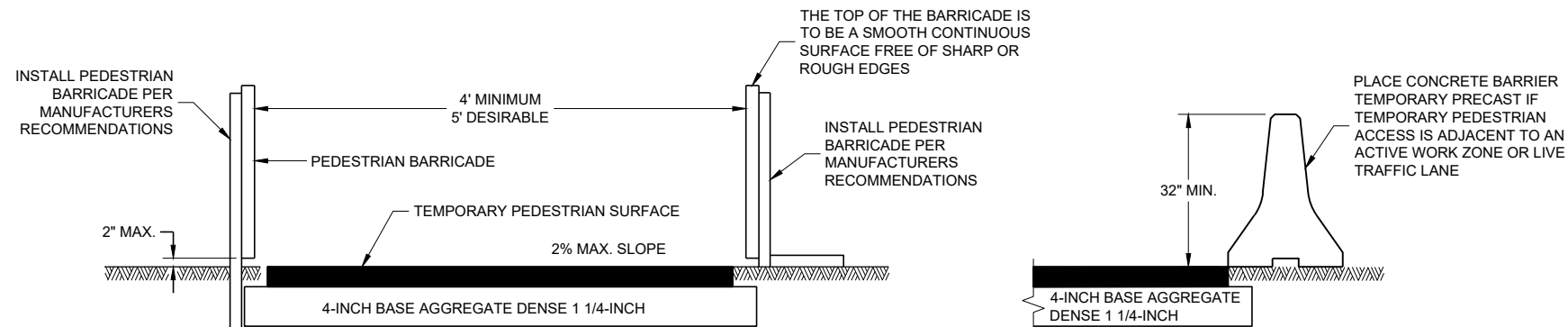
TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2020
DATE
STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER
/S/ Andrew Heidtke
FHWA



NARROW SIDEWALK PASSING DETAIL



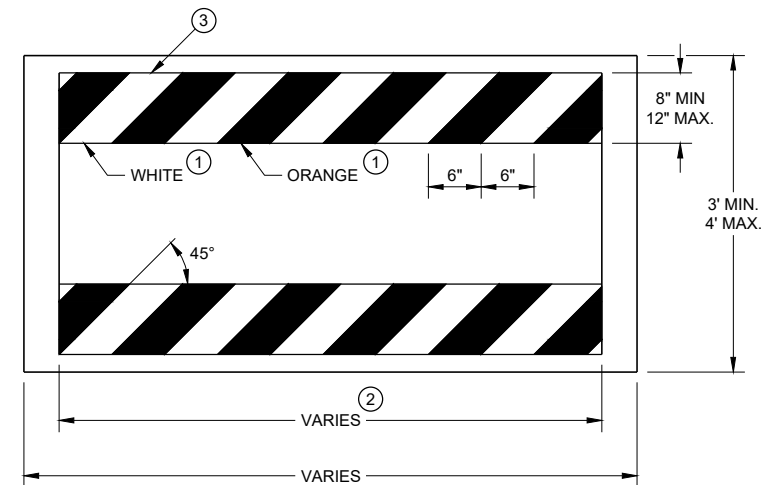
TEMPORARY PEDESTRIAN ACCESS

GENERAL NOTES

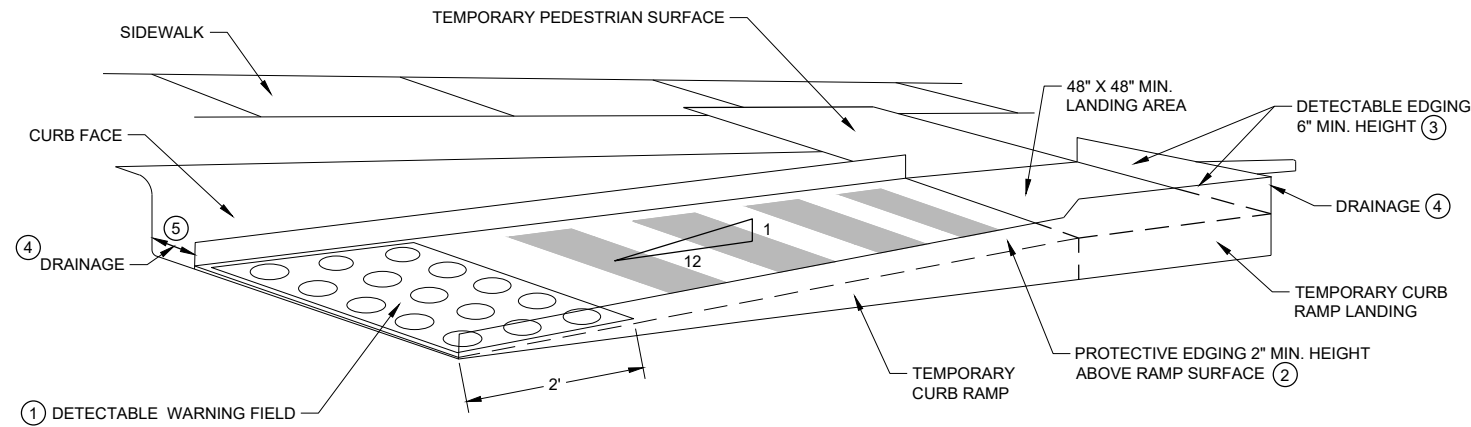
BARRICADE DEVICE SELECTED FROM APPROVED PRODUCT LIST

- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② SHEETING REQUIRED ON MORE THAN 50% OF BARRICADE WIDTH.
- ③ PLACE SHEETING ON BOTH SIDES OF THE BARRICADE.

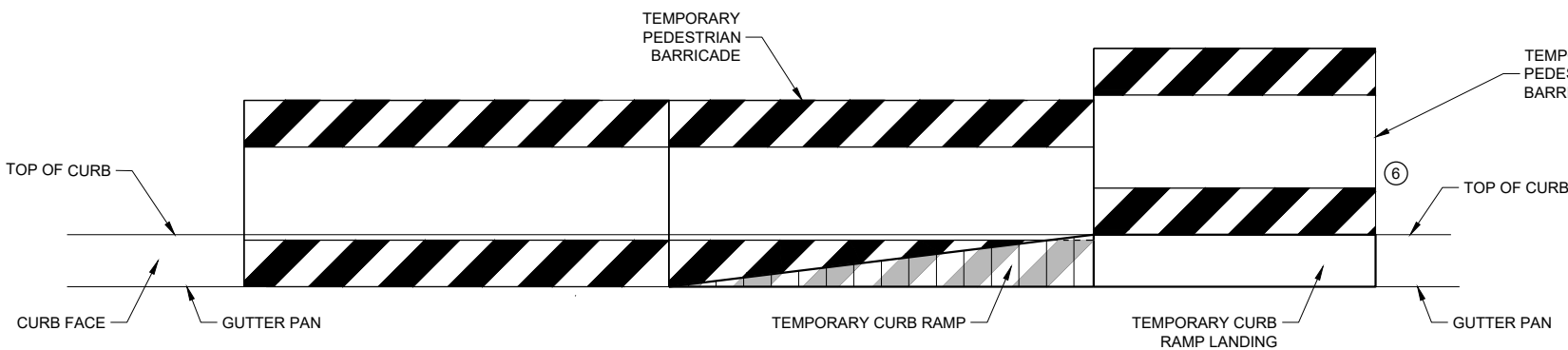
★ USE THIS DETAIL FOR SHEETING PLACEMENT REFERENCE.



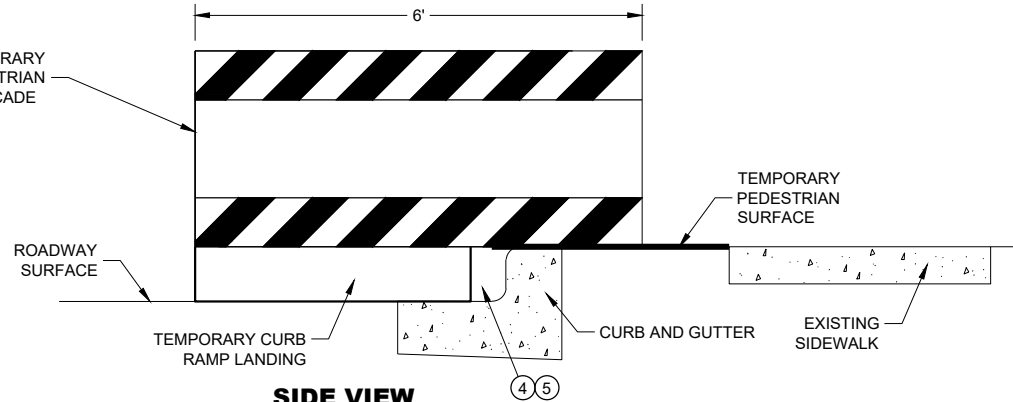
TEMPORARY PEDESTRIAN BARRICADE*



PERSPECTIVE VIEW



FRONT VIEW

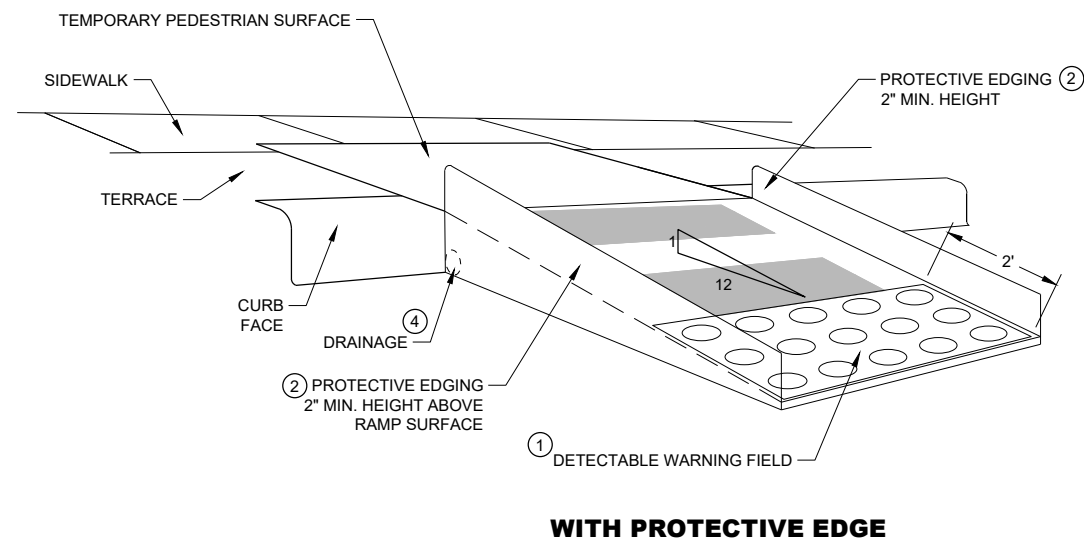
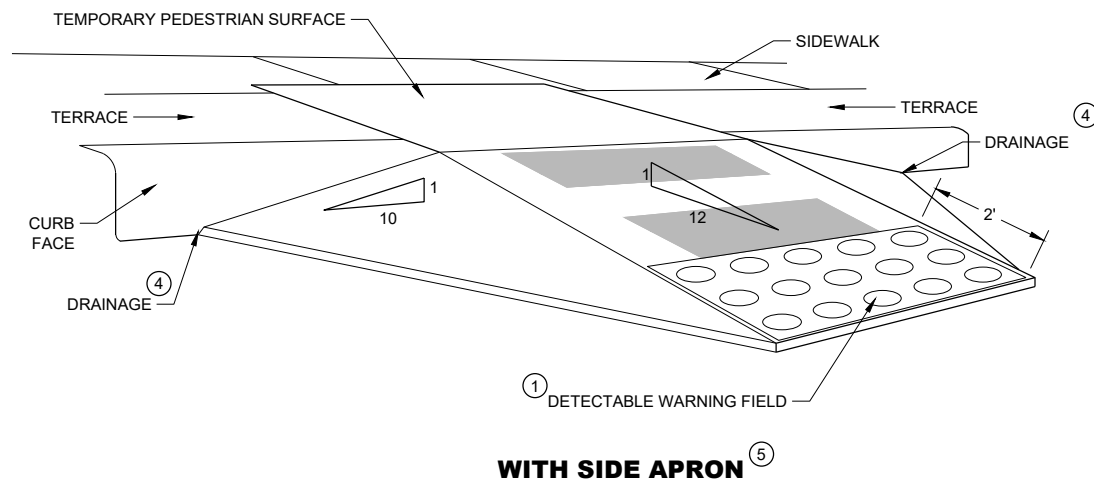


SIDE VIEW

TEMPORARY CURB RAMP PARALLEL TO CURB

GENERAL NOTES

- CURB RAMPS SHALL BE 48" MIN. WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE.
- CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.
- CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP. LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.
- CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES MAY BE VERTICAL UP TO 1/4" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".
- ① INSTALL CONTRASTING TEMPORARY DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS, AS SHOWN IN THE PLANS.
- ② PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- ③ DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- ④ DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ⑤ ENSURE CURB RAMP IS OUT OF THE GUTTER PAN.
- ⑥ IF ONLY PART OF THE END PANEL OF TEMPORARY PEDESTRIAN BARRICADE PANEL IS NEEDED, EXTEND EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL HERE.



TEMPORARY CURB RAMP PERPENDICULAR TO CURB

GENERAL NOTES

CURB RAMPS SHALL BE 48" MINIMUM WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE.

ALTERNATE SIDEWALK WORK BETWEEN LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.

CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.

CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.

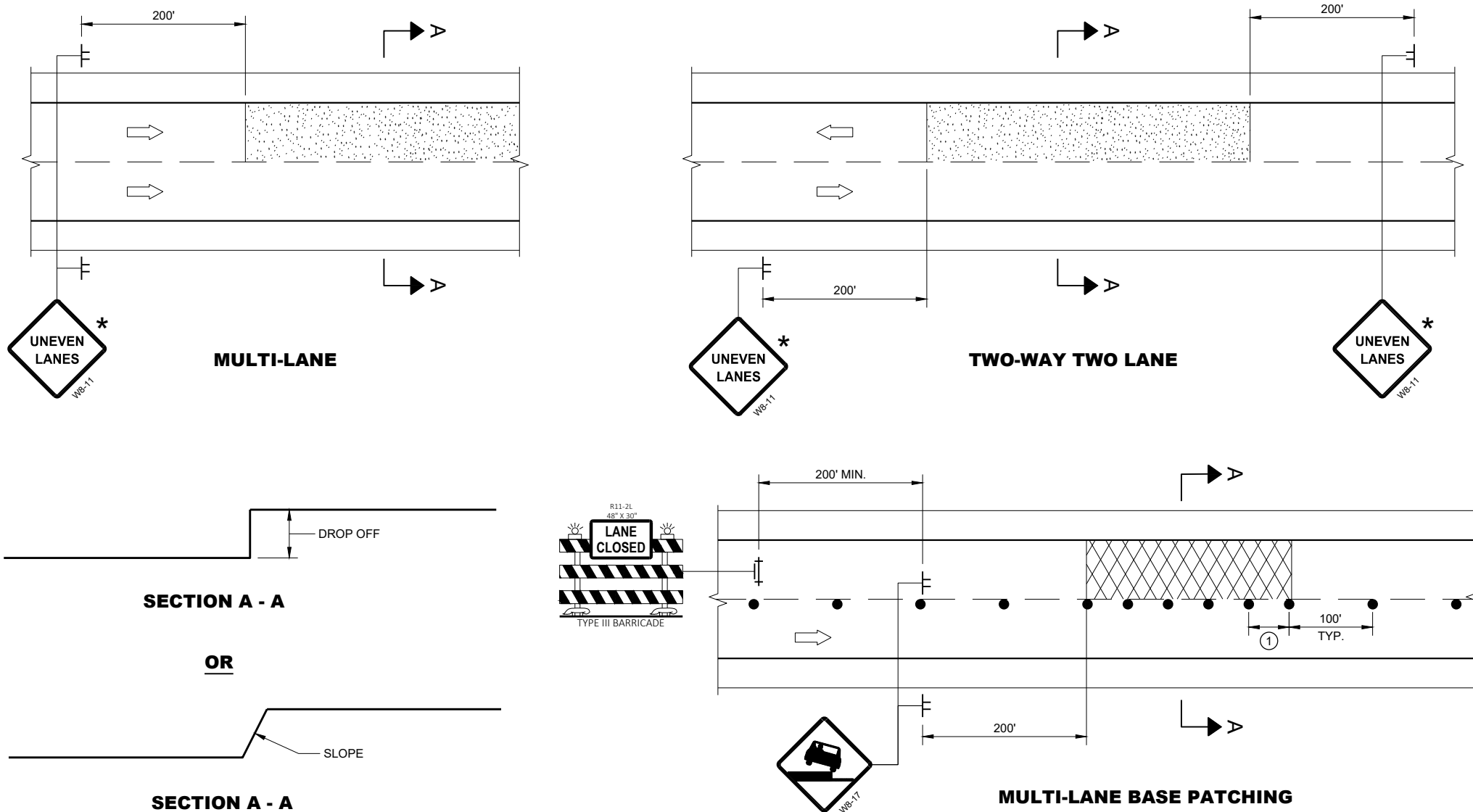
LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN ½" WIDTH.

CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED ½". LATERAL EDGES MAY BE VERTICAL UP TO ¼" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN ¼" AND ½".

- ① INSTALL CONTRASTING TEMPORARY DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS, AS SHOWN IN THE PLANS
- ② PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- ③ DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- ④ DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ⑤ CAN ONLY BE USED FOR RAMPS WITH 6" OR LESS OF VERTICAL CHANGE.

**TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



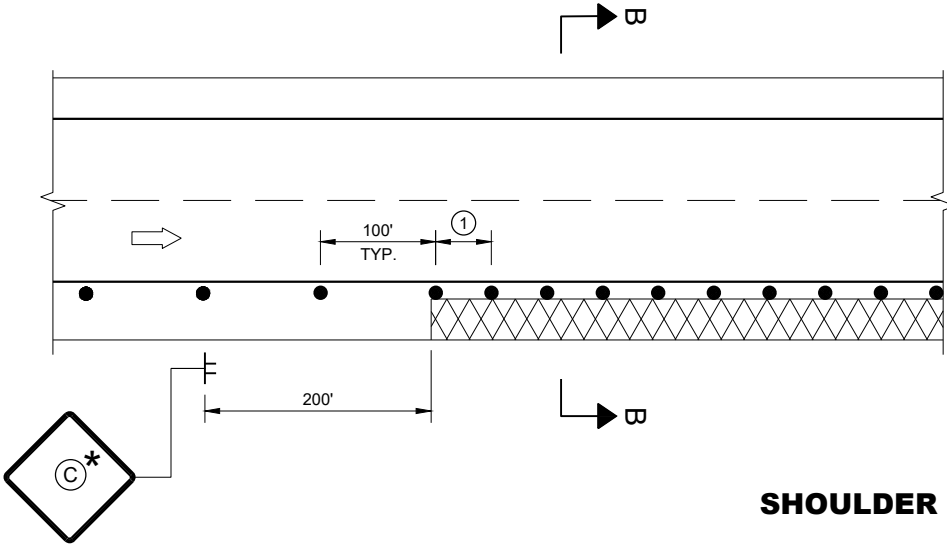
ADJACENT LANE DROP-OFFS

GENERAL NOTES

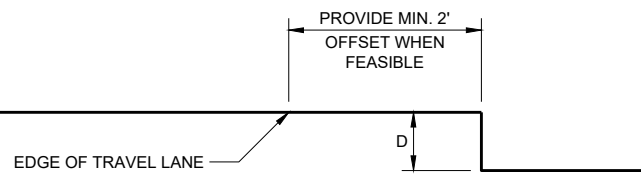
- FOR SPOT LOCATIONS USE ENGINEERING JUDGEMENT WHEN PLACING ADDITIONAL SIGNS.
- ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.
- "WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.
- WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.
- * IF THE DROP-OFF IS CONTINUOUS ALONG THE PROJECT, PLACE ADDITIONAL SIGNS EVERY 1 MILE AND AFTER EVERY ENTRANCE RAMP.
- ① USE CLOSER SPACING WHEN DELINEATING DROP-OFF.

LEGEND

- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL DRUM
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- DIRECTION OF TRAFFIC
- WORK AREA WITH DROP-OFF
- MILLED SURFACE



SHOULDER DROP-OFFS



SECTION B - B

D	SIGN ③
< 2" WITH A SLOPE STEEPER THAN 3:1	LOW SHOULDER WO8-9
2" < 6" WITH A SLOPE STEEPER THAN 3:1	SHOULDER DROP - OFF W8-9A PROVIDE A 3:1 OR FLATTER SLOPE OF MATERIAL ADJACENT TO THE PAVEMENT

TRAFFIC CONTROL,
DROP-OFF SIGNING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
March 2018
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

GENERAL NOTES

DRAWING NOT TO SCALE. ALL SIGNS AND POSTS ON THIS SHEET SHALL BE PAID FOR WITH 'TRAFFIC CONTROL SIGNS' BID ITEM. ALL SIDE ROADS WHICH ARE UNDER CONSTRUCTION OF CURB AND GUTTER AND/OR GRADING SHALL BE ADEQUATELY SIGNED.

ALL SIGNS AND DEVICES SHALL BE IN CONFORMANCE WITH THE WISCONSIN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD). SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISDOT STANDARD SIGN PLATES.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THAT THE BACKGROUND IS ORANGE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.



THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

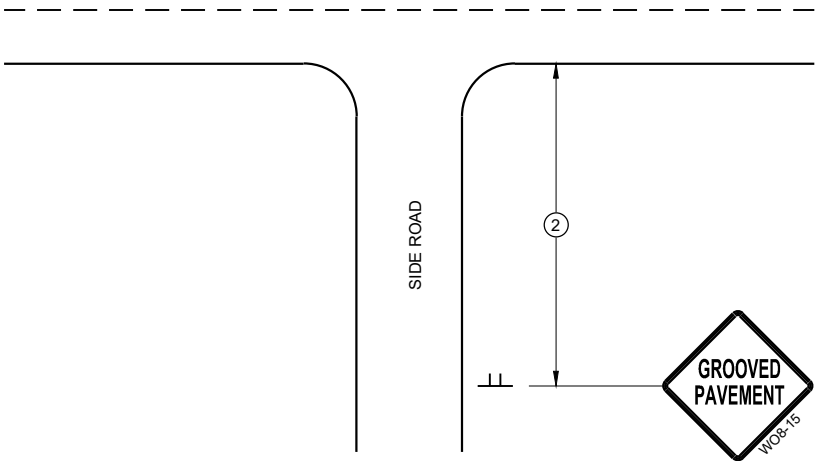
ALL SIGNS INAPPROPRIATE TO THE STATUS OF THE CONTROL ZONE, INCLUDING PRE-EXISTING SIGNS IN THE VICINITY, SHALL BE COVERED OR REMOVED AS DIRECTED BY THE ENGINEER.

SEE 15C34 FOR ADDITIONAL TRAFFIC CONTROL SIGNING WHEN CENTERLINE PAVEMENT MAKINGS ARE MISSING. 'DO NOT PASS' SIGNS MUST BE INSTALLED ON THE SAME DAY AS MILLING OPERATIONS.

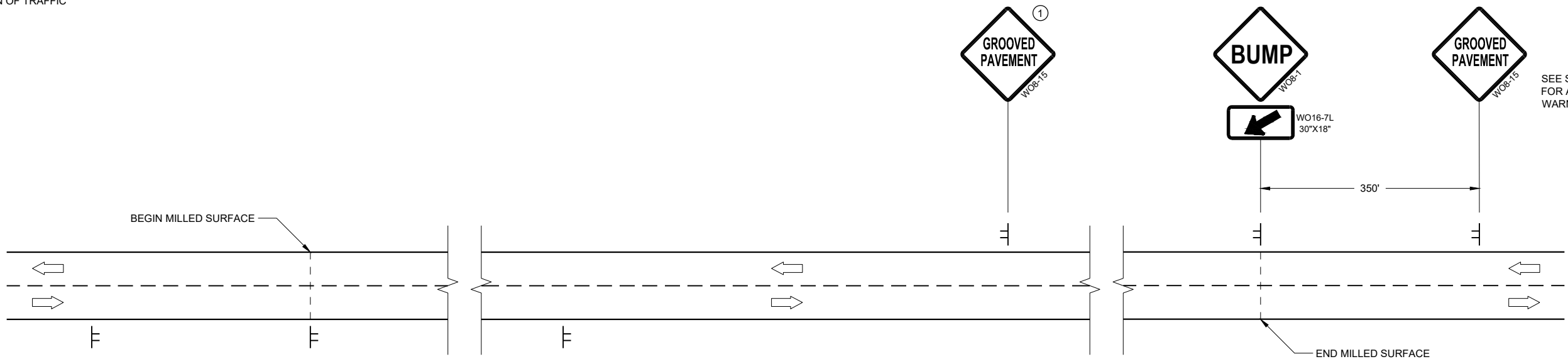
- 1 PLACE SIGNS 350' IN ADVANCE OF MILLED SURFACES AND AT 1 MILE INTERVALS, OR AS DIRECTED BY THE ENGINEER.
- 2 PLACE SIGN 200' MIN. FROM INTERSECTION AND 200' MIN. AFTER ADVANCE WARNING SIGN SHOWN IN SDD 15C04.

LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  DIRECTION OF TRAFFIC



TYPICAL SIDE ROAD APPROACH
SIGN DETAIL



SEE SDD15C04
FOR ADVANCE
WARNING SIGNS

SEE SDD15C04
FOR ADVANCE
WARNING SIGNS

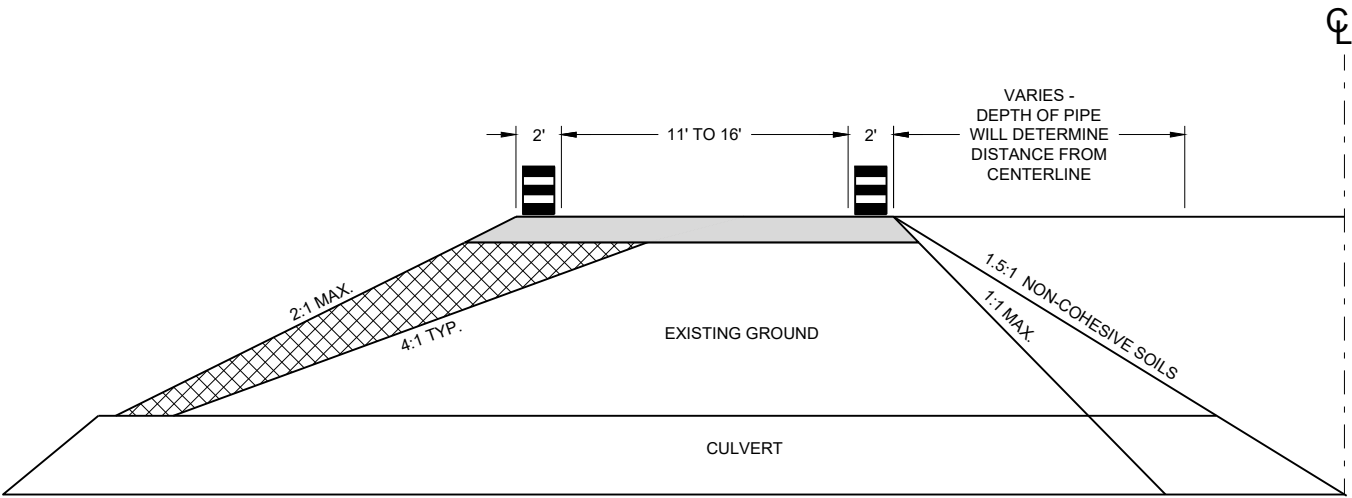
DETAIL FOR SIGNING ON MILLED SURFACES

TRAFFIC CONTROL,
SIGNING ON ROADWAYS
WITH MILLED SURFACES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA



CROSS SECTION

GENERAL NOTES

USE 1:1 FOR COHESIVE CLAYS AND SILTS, LOAMS, SANDY CLAYS AND ANGULAR GRAVEL SOILS.
USE 1.5:1 FOR NON-COHESIVE SOILS.

THE TAPER SHOULD EXTEND ACROSS THE SHOULDER UNLESS DOING SO WOULD GREATLY CONFLICT WITH THE WORK OPERATION.

ALL LANE CLOSURE SIGNS SHALL BE REMOVED OR COVERED AND ALL DEVICES REMOVED BEYOND THE SHOULDER WHEN WORK IS NOT IN PROGRESS AND THE LANE IS RESTORED TO A SAFE OPERATING CONDITION.

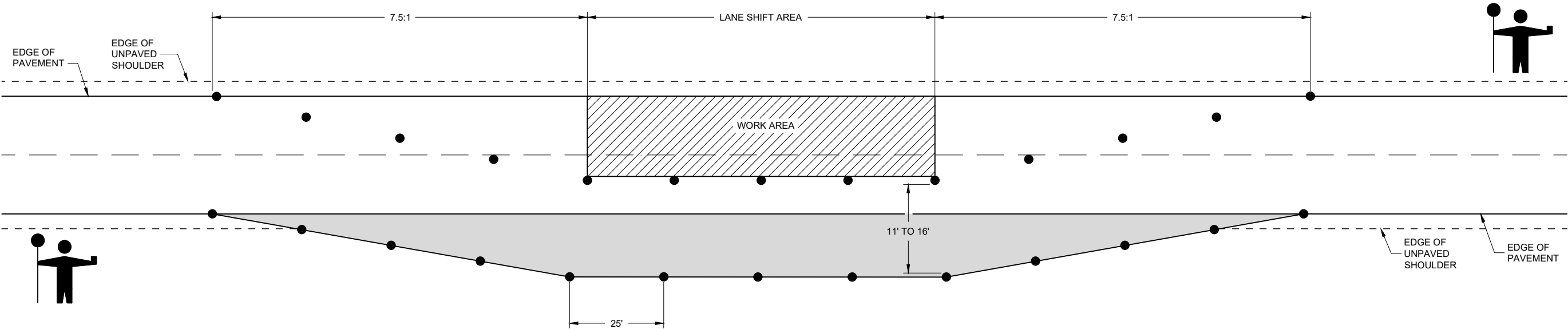
CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

USE WITH SDD 15C12 "TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATIONS"

USE WITH SDD 15D45 "SIGNING ON ROADWAYS WITH LOOSE GRAVEL"

LEGEND

- DRUM WITHOUT WARNING LIGHT
- 6" BASE AGGREGATE DENSE 1 1/4" - INCIDENTAL TO LANE SHIFT ITEM
- FILL - INCIDENTAL TO LANE SHIFT ITEM
- WORK AREA
- FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF



LANE SHIFT IN FLAGGING OPERATION

**TRAFFIC CONTROL,
TEMPORARY LANE SHIFT
DURING CULVERT WORK**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2021
DATE


/S/ Andrew Heidtke
WORK ZONE ENGINEER


FHWA


LEGEND


- V1

WORK VEHICLE
- V2

SHADOW VEHICLE
- 

TRUCK MOUNTED ATTENUATOR (TMA)
- 

FLASHING ARROW PANEL (CAUTION)
- 

WORK AREA
- 

DIRECTION OF TRAFFIC

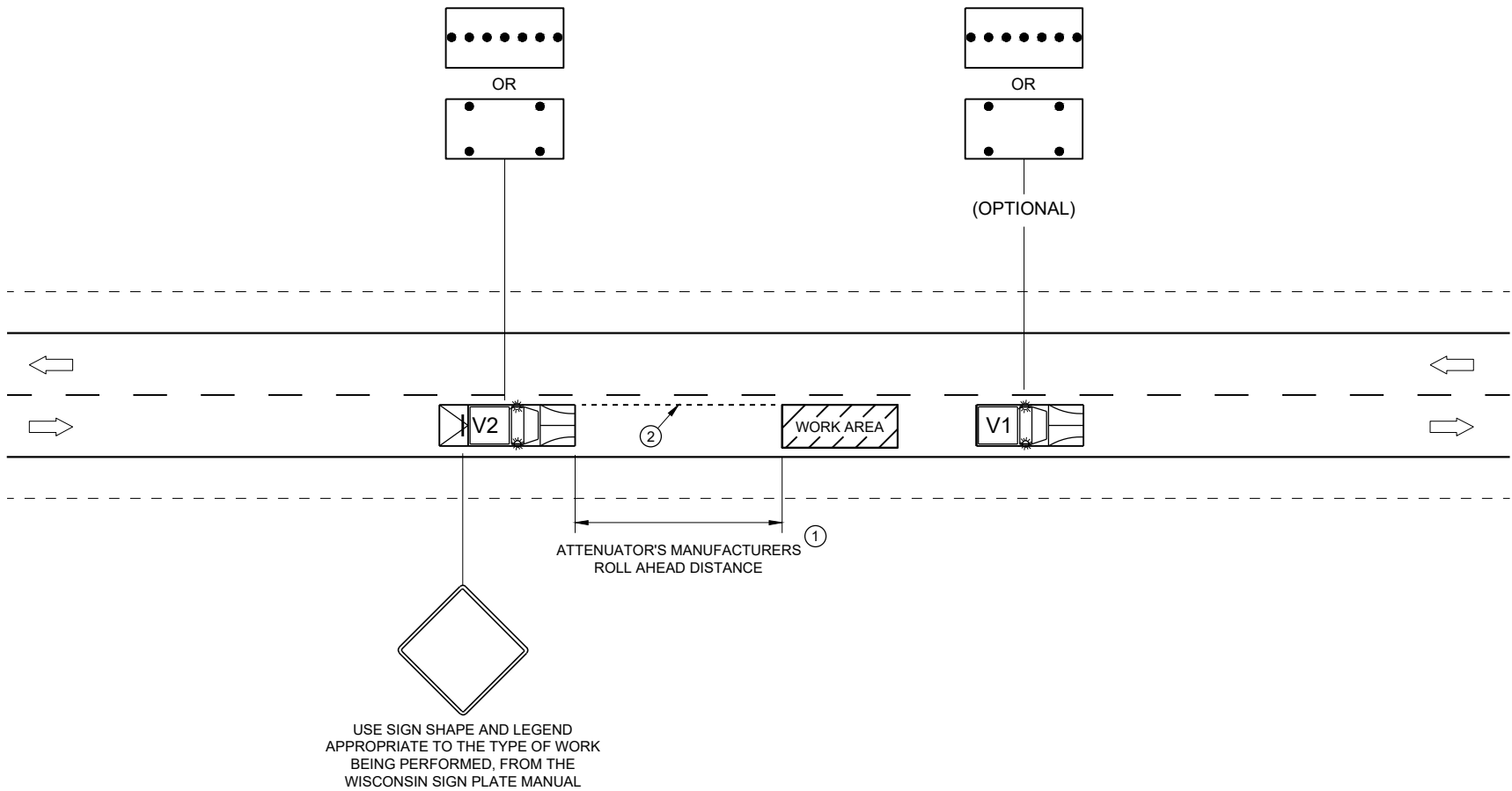
POSTED SPEED PRIOR TO WORK STARTING (MPH)	DECISION SIGHT DISTANCE (D)
0 - 25	550'
30	550'
35	700'
40	700'
45	900'
50	900'
55	1200'

GENERAL NOTES

- ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.
- MOBILE IS WORK THAT MOVES CONTINUOUSLY OR MOVES AT LEAST THE DECISION SIGHT DISTANCE EVERY 15 MINUTES.
- ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.
- ALL ARROW PANELS SHALL BE REAR FACING, TYPE "B" OR "C", AND DISPLAYING THE FLASHING CAUTION MODE. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.
- USE AN ATTENUATOR ON THE REARMOST VEHICLE THAT BLOCKS ALL OR PART OF THE TRAFFIC LANE.
- ①

DISTANCE BETWEEN VEHICLES MAY INCREASE FROM THE ATTENUATOR'S ROLL AHEAD BASED ON TERRAIN, SIGHT DISTANCE, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.
- ②

ALIGN LEFT SIDE OF SHADOW VEHICLE WITH EDGE OF WORK AREA.



TRAFFIC CONTROL,
MOBILE OPERATIONS ON
AN UNDIVIDED ROADWAY

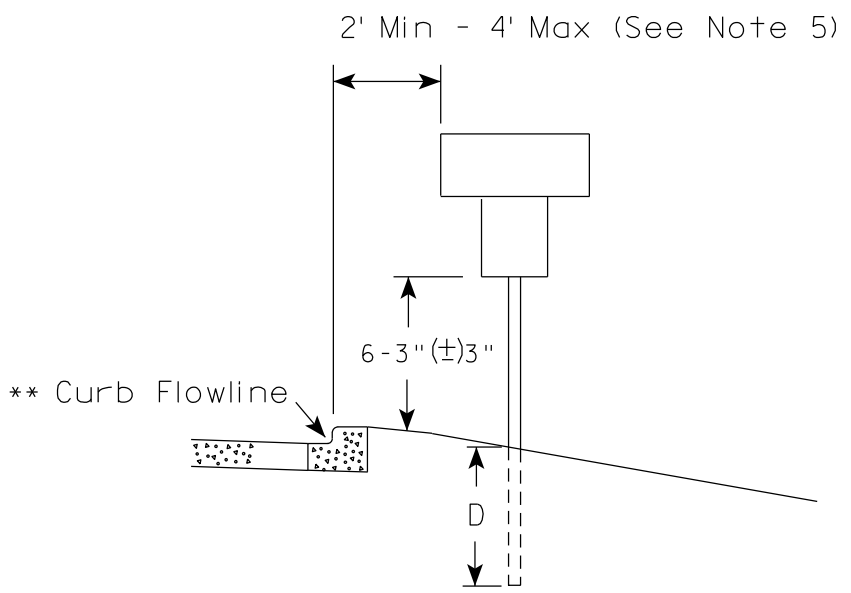
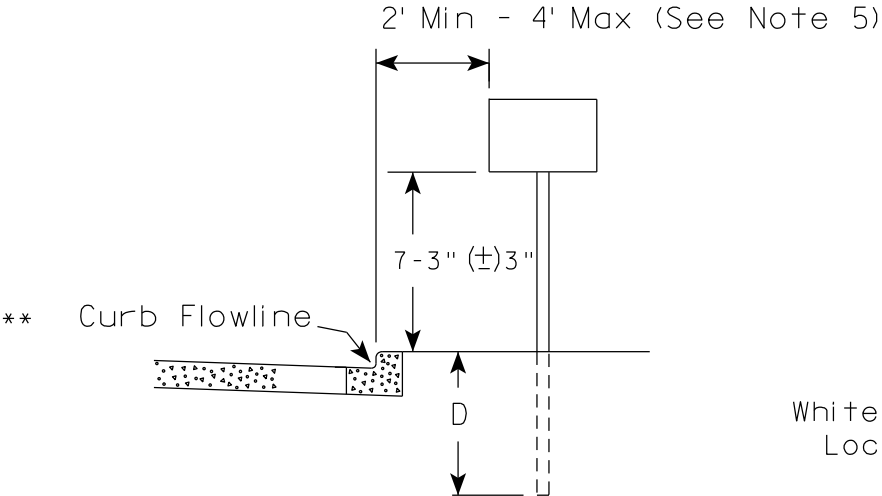
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2021
DATE

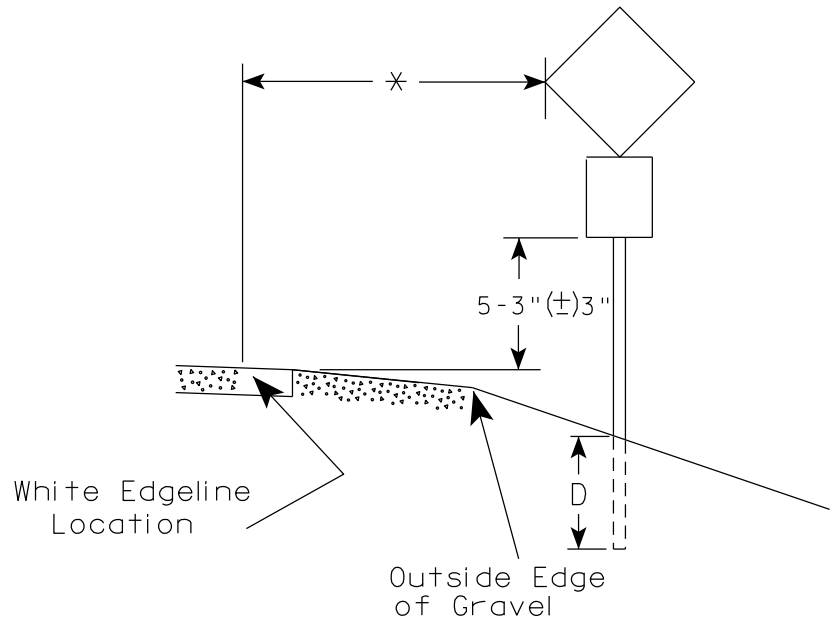
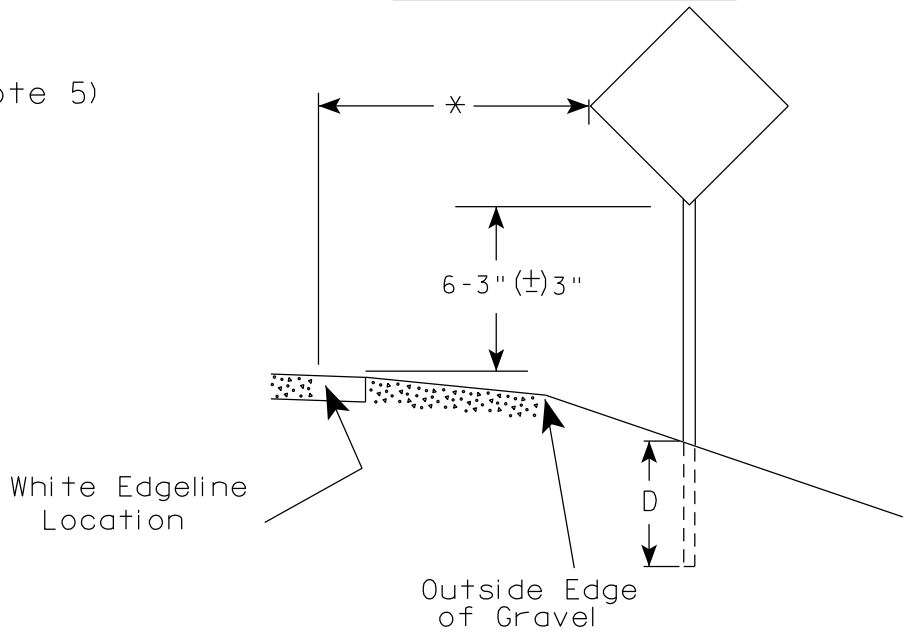
/S/ Andrew Heidtke
STATEWIDE WORK ZONE TRAFFIC
SAFETY ENGINEER

FHWA

URBAN AREA



RURAL AREA (See Note 2)



POST EMBEDMENT DEPTH	
Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

GENERAL NOTES

1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on or behind barrier wall, see A4-10 sign plate.
The Double Arrow sign (W12-1D) shall be mounted at a height of 2'-3" (±) 3". The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±) 3".
3. For expressways and freeways, mounting height is 7'- 3" (±) 3" or 6'-3" (±) 3" depending upon existence of a sub-sign.
4. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±) 3".
5. Offset distance shall be consistent with existing signs or consistent throughout length of project.
6. Folding signs shall be mounted at a height of 5'-3" (±) 3" or as directed by the Engineer.

* * The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

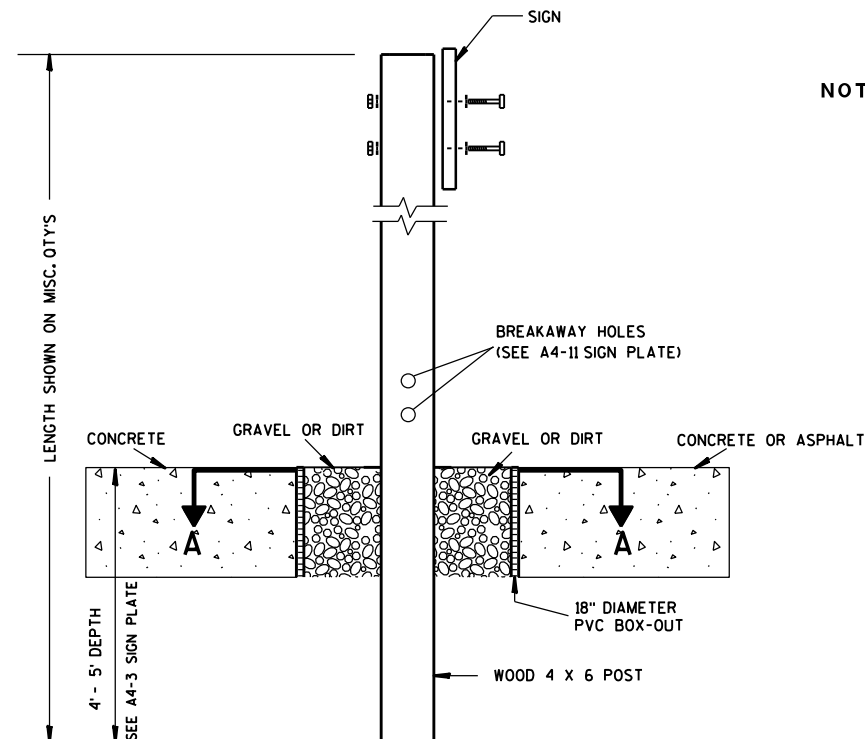
* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

TYPICAL INSTALLATION
OF PERMANENT TYPE II
SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

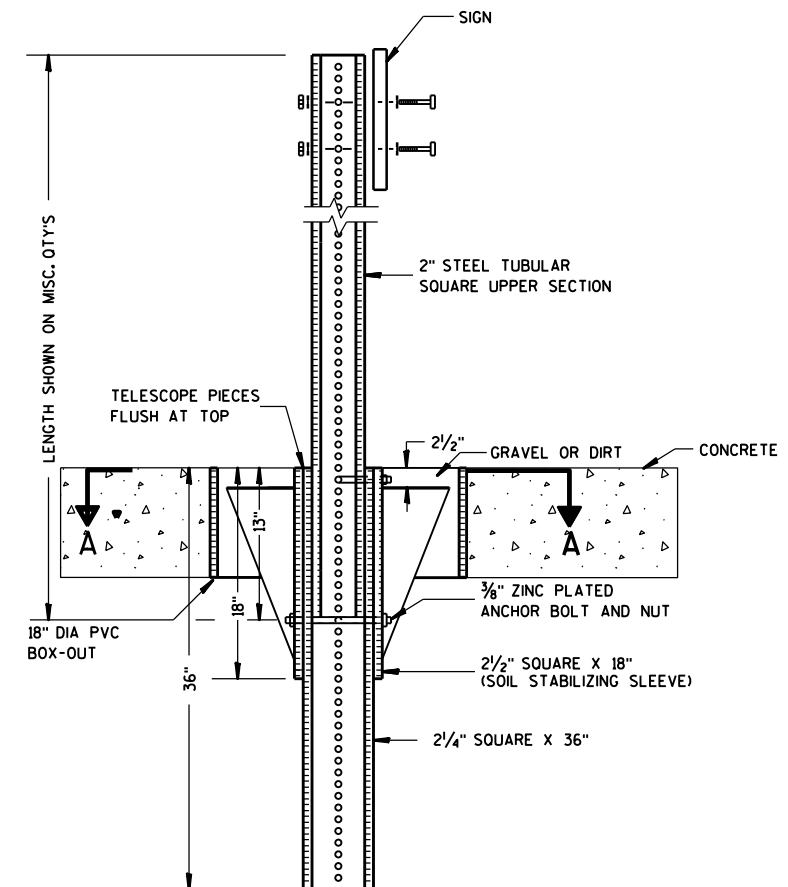
DATE 12/6/23 PLATE NO. A4-3.23



ELEVATION VIEW

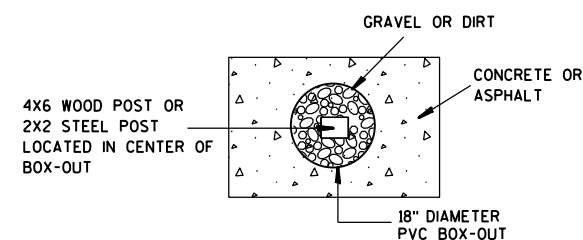
DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

- NOTES:**
1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
 2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
 3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



PLAN VIEW

FOR NEW CONCRETE/ASPHALT INSTALLATIONS

**SIGN POST
BOX-OUTS
A4-3B**

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 1/27/14 PLATE NO. A4-3B.1

PROJECT NO:

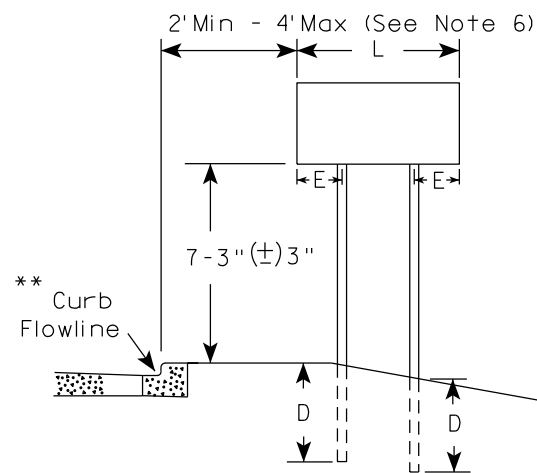
HWY:

COUNTY:

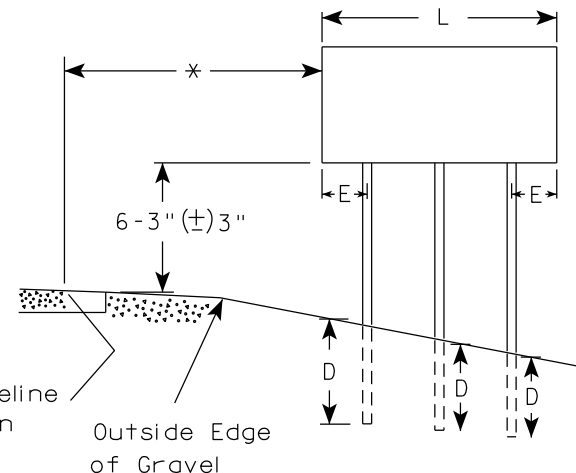
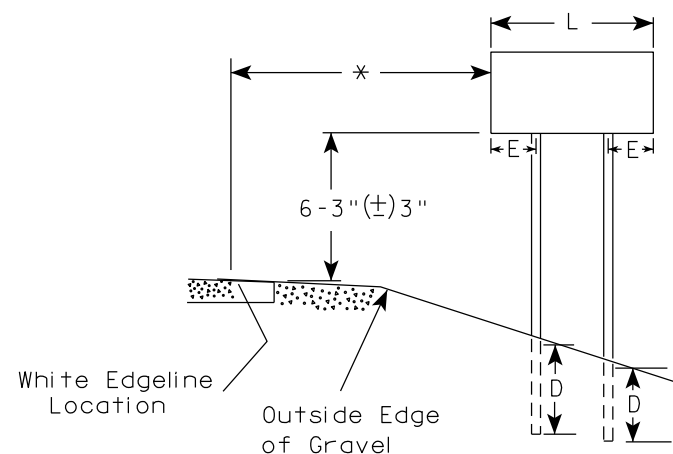
SHEET NO:

E

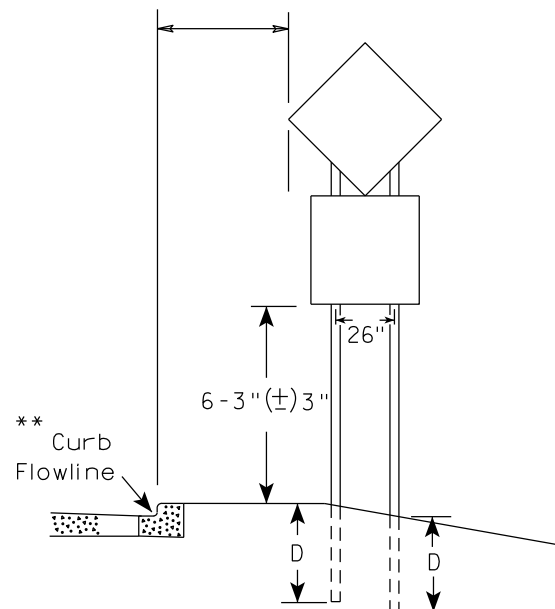
URBAN AREA



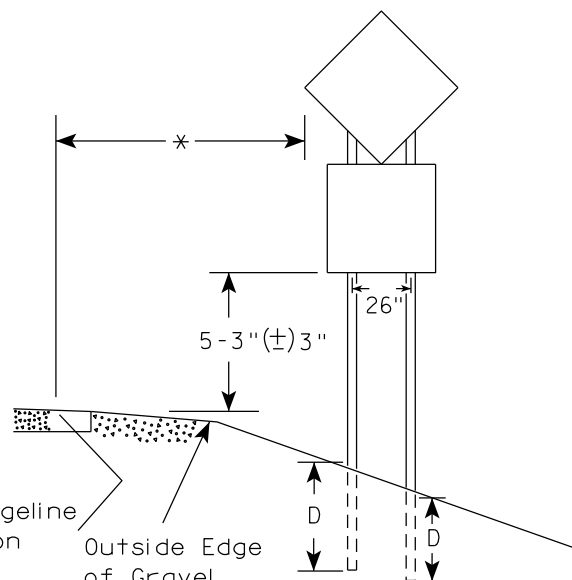
RURAL AREA (See Note 3)



2' Min - 4' Max (See Note 6)



48" DIAMOND WARNING SIGN



48" DIAMOND WARNING SIGN

SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)	
L	E
Greater than 48" Less than 60"	12"
60" to 108"	L/5

SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)	
L	E
Greater than 108" to 144"	12"

POST EMBEDMENT DEPTH

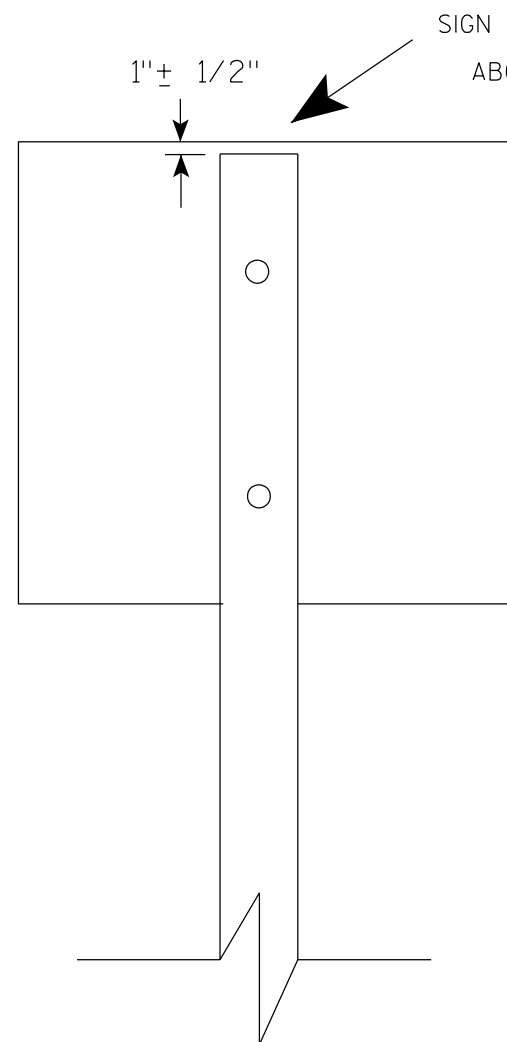
Area of Sign Installation (Sq.Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION
OF TYPE II SIGNS
ON MULTIPLE POSTS

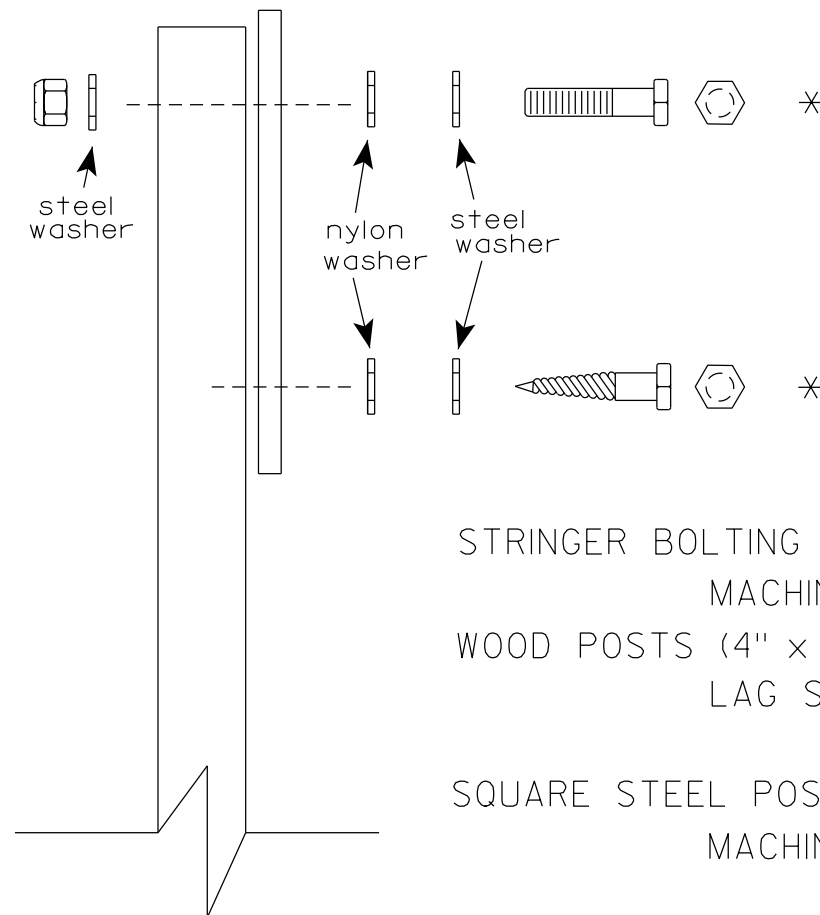
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 12/6/23 PLATE NO. A4-4.16



SIGN SHALL BE MOUNTED TO PROJECT
ABOVE THE TOP OF THE POST



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

- a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
- b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)

MACHINE BOLTS - $\frac{5}{16}$ " X 1-3/4" Length w/ lock nuts

WOOD POSTS (4" x 6")

LAG SCREWS - $\frac{3}{8}$ " X 3" (NO STRINGERS ON BACK OF SIGN)
 $\frac{3}{8}$ " X 4" (STRINGERS ON BACK OF SIGN)

SQUARE STEEL POSTS (2" x 2")

MACHINE BOLTS - $\frac{3}{8}$ " X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)
 $\frac{3}{8}$ " X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)

RIVETS - $\frac{9}{32}$ " (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH

WASHERS (ALL POSTS) -

1-1/4" O.D. X $\frac{3}{8}$ " I.D. X $\frac{1}{16}$ " STEEL

1-1/4" O.D. X $\frac{3}{8}$ " I.D. X .080 NYLON

* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

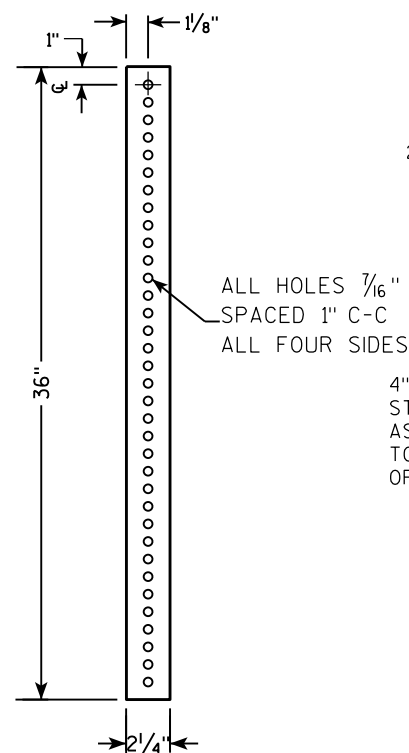
ATTACHMENT OF SIGNS TO POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R. Rauch
For State Traffic Engineer

DATE 4/1/2020 PLATE NO. A4-8.9

**2 1/4 " SQUARE
12 GAUGE
PERFORATED
GALVANIZED FINISH**



2 1/2" TELESPAR TUBE

4" x 10" x 10 GA. STEEL PLATE (CUT AS SHOWN) WELDED TO ALL FOUR CORNERS OF TELESPAR TUBE

4"

2 1/2"

10"

3 1/2"

18"

TECHNICAL DRAWING OF A VERTICAL SIGNPOST ASSEMBLY.

Labels and Dimensions:

- 18" DIA SCHEDULE 40 PVC BOX-OUT**: Dimensioned as 36" (height) and 18" (width).
- TELESCOPE PIECES FLUSH AT TOP**: Dimensioned as 13" (height).
- 2" STEEL TUBULAR SQUARE UPPER SECTION**: Dimensioned as 2 1/2" (width).
- 2 1/4" SQUARE X 36"**: Dimensioned as 36" (height).
- 2 1/2" SQUARE X 18" (SOIL STABILIZING SLEEVE)**: Dimensioned as 18" (width).
- 3/8" ZINC PLATED CORNER ANCHOR BOLT AND NUT**: Dimensioned as 2 1/2" (width).
- 3/8" ZINC PLATED ANCHOR BOLT AND NUT**: Dimensioned as 2 1/2" (width).
- 2 1/2" GRAVEL OR DIRT**: Dimensioned as 2 1/2" (width).
- ALL HOLES 7/16" SPACED 1" C-C ALL FOUR SIDES**: Dimensioned as 1" (spacing).
- SEE SIGN PLATE A4-8 FOR BOLT WASHER, & NUT MATERIAL**: Dimensioned as 1" (spacing).
- SIGN**: Dimensioned as 1" (width).


Notes:

- ALL HOLES 7/16" SPACED 1" C-C ALL FOUR SIDES
- SEE SIGN PLATE A4-8 FOR BOLT WASHER, & NUT MATERIAL
- TELESCOPE PIECES FLUSH AT TOP
- 2" STEEL TUBULAR SQUARE UPPER SECTION
- 2 1/2" GRAVEL OR DIRT
- 3/8" ZINC PLATED CORNER ANCHOR BOLT AND NUT
- 3/8" ZINC PLATED ANCHOR BOLT AND NUT
- 2 1/2" SQUARE X 18" (SOIL STABILIZING SLEEVE)
- 2 1/4" SQUARE X 36"
- 18" DIA SCHEDULE 40 PVC BOX-OUT

TECHNICAL DRAWING OF A SIGNPOST ASSEMBLY:

- TELESCOPE PIECES FLUSH AT TOP**: Indicated by a dimension line on the left.
- 2" STEEL TUBULAR SQUARE UPPER SECTION**: The main vertical support.
- ALL HOLES $\frac{7}{16}$ " SPACED 1" C-C ALL FOUR SIDES**: Specification for the perforations in the tubular section.
- SIGN**: Attached to the top of the post.
- SEE SIGN PLATE A4-8 FOR BOLT WASHER, & NUT MATERIAL**: Reference to a separate plate for hardware details.
- 3/8" ZINC PLATED CORNER ANCHOR BOLT AND NUT**: Hardware used to secure the post to the base.
- 1"**: Dimension for the offset of the anchor bolt from the post's centerline.
- 3/8" ZINC PLATED ANCHOR BOLT AND NUT**: Hardware used to secure the base plate to the ground.
- 2 1/2" SQUARE X 18" (SOIL STABILIZING SLEEVE)**: The base plate of the post.
- 2 1/4" SQUARE X 36"**: The main base plate.
- Dimensions**:
 - Overall height of the post: 36"
 - Height from base to top of post: 18"
 - Height from base to top of post: 12"
- Arrows A**: Indicate downward forces or weights applied to the post.

DIRECTION
OF TRAFFIC



SECTION A-A

Area of Sign Installation (Sq. Ft.)	Number of Required Posts
9 or less	1
Greater than 9 less than or equal to 18	2
Greater than 18 less than or equal to 27	3

Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

TUBULAR STEEL
SIGN POST
A4-9

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch

for State Traffic Engineer

DATE 2/05/15 PLATE NO. A4-9.9

PROJECT NO:

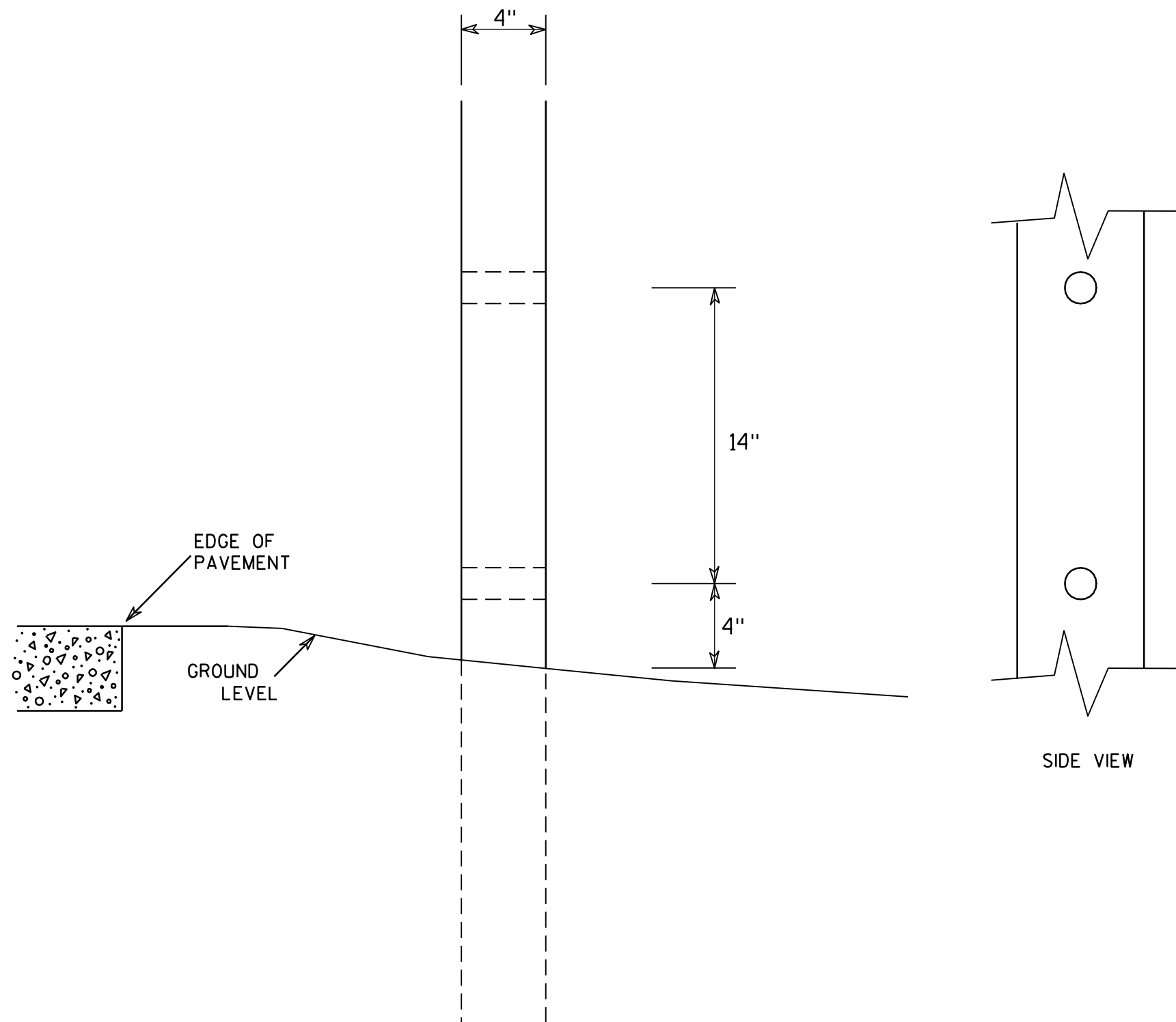
HWY:

COUNTY:

SHEET NO:

T

7

GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

7

**4 X 6 WOOD POST
MODIFICATIONS**

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Chester J. Spang
for State Traffic Engineer

DATE 3/27/97

PLATE NO. A4-11.2

PROJECT NO:

HWY:

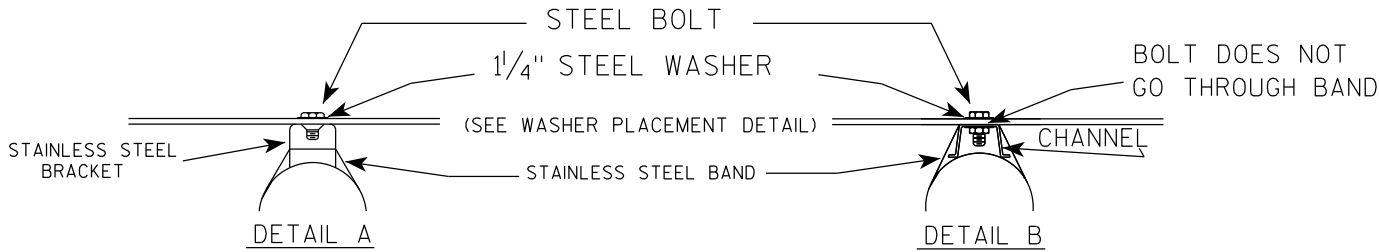
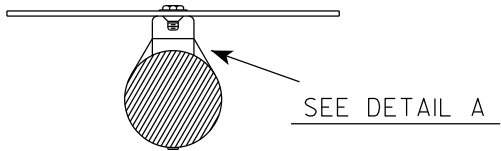
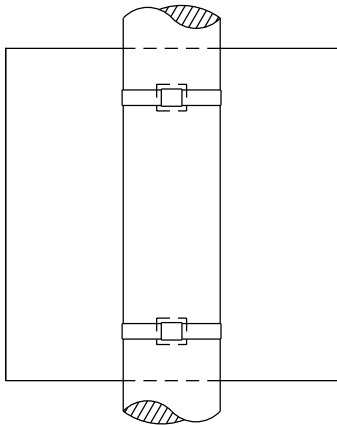
COUNTY:

SHEET NO:

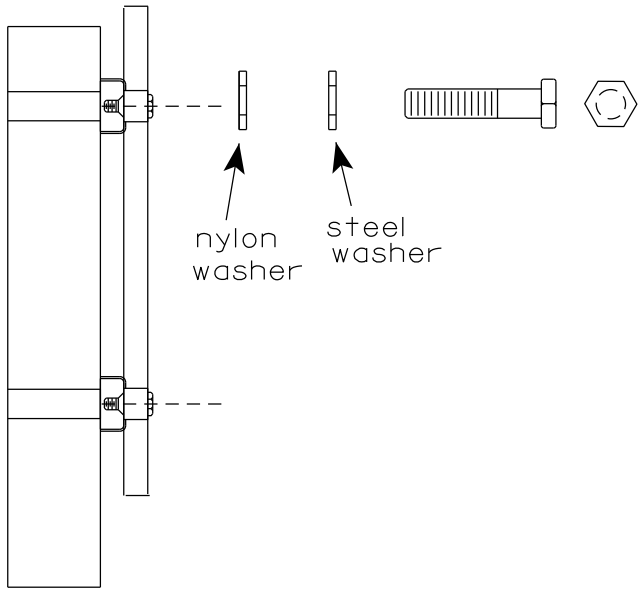
E

BANDING

SINGLE SIGN



WASHER PLACEMENT

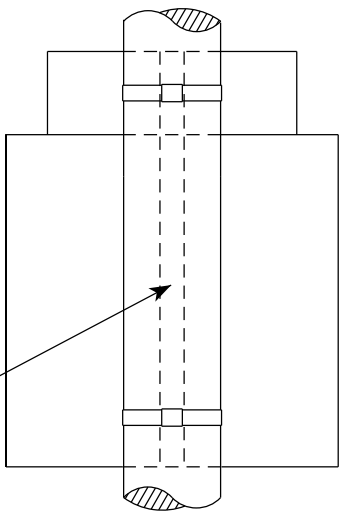


WASHERS (ALL POSTS) -
1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
1-1/4" O.D. X 3/8" I.D. X .080 NYLON
FOR ALL TYPE H SIGNS

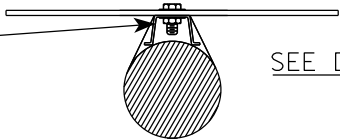
GENERAL NOTES

1. Any sign over 3 feet in width shall use the V-Block banding method. See A5-10 standard plate.
2. Signs 3 feet or greater in height shall have three bracket bands installed. Signs less than 3 feet in height shall have two bracket bands installed.
3. Banding and assembly bracket shall be stainless steel. All bands shall be 3/4" in width and 0.025" thickness.
4. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
 - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
 - b. Electro-galvanized in accordance with ASTM designation: B 633, Type III, SC 3

"J" ASSEMBLY



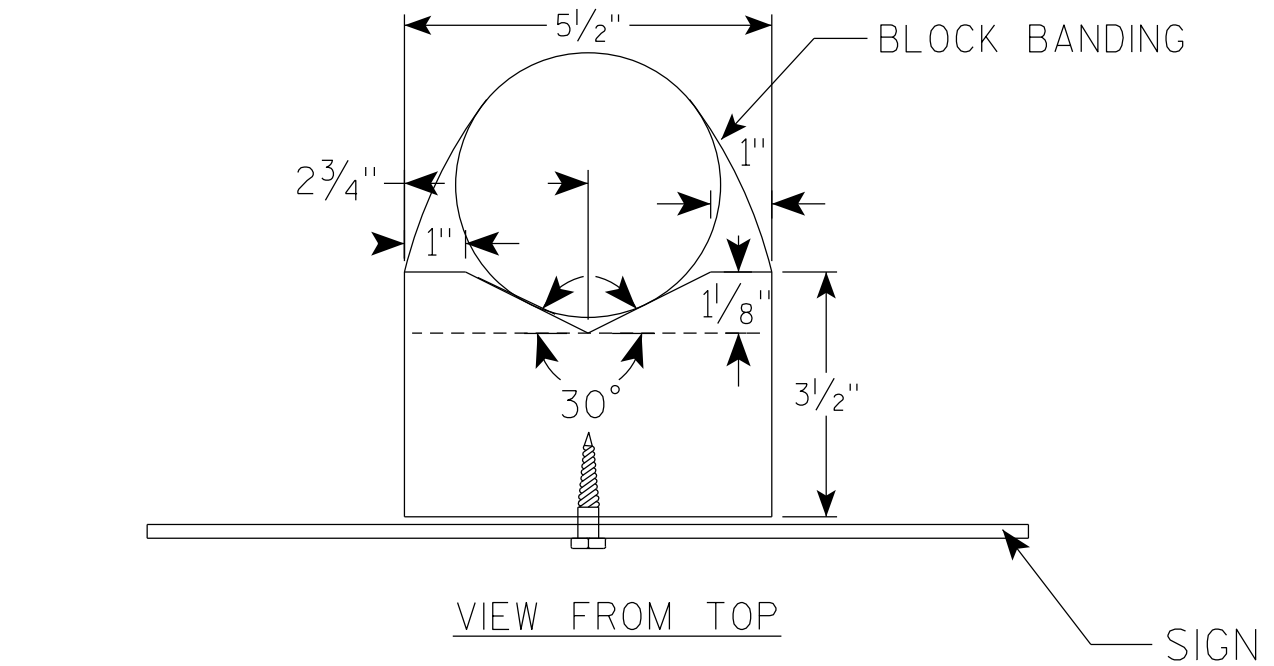
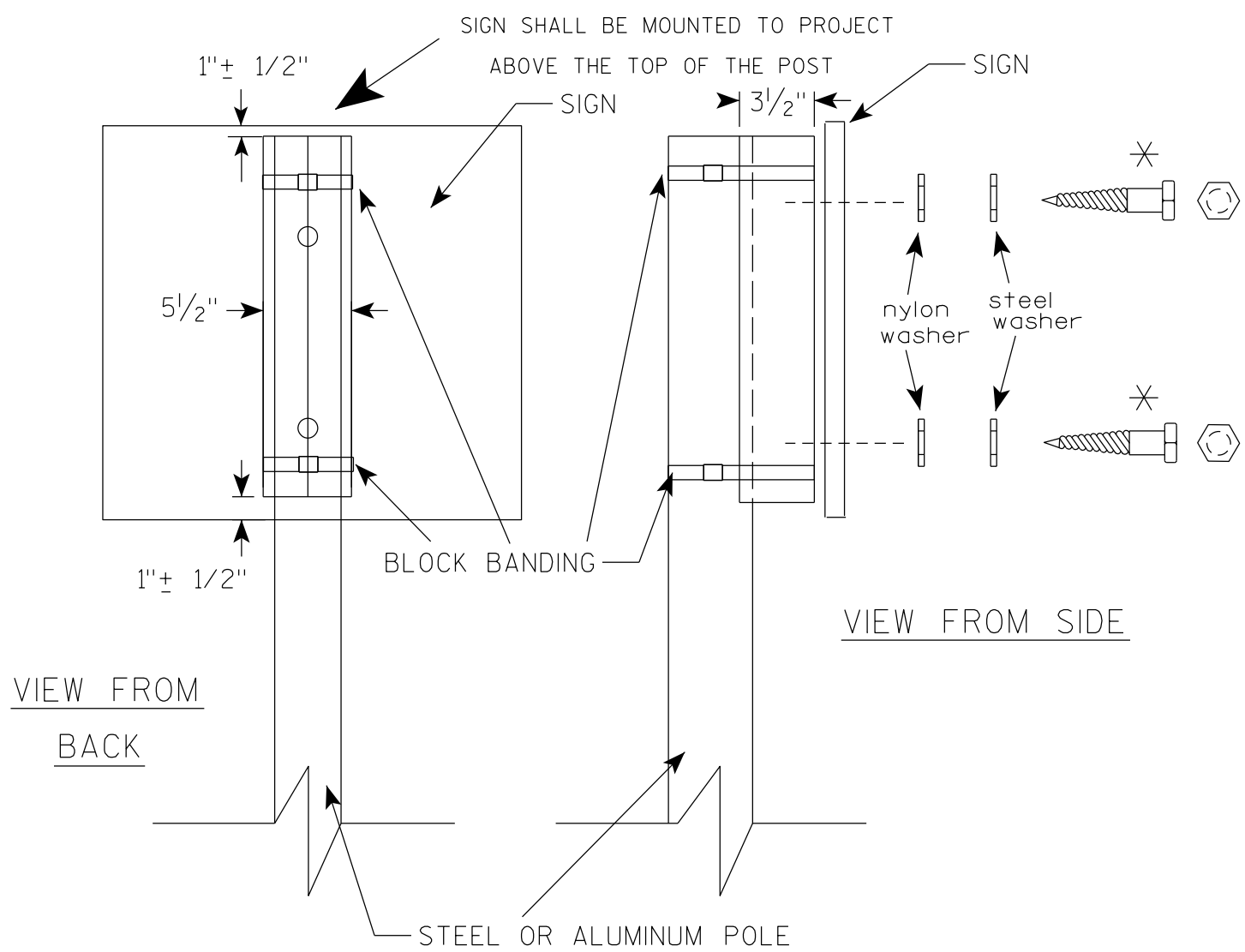
CHANNEL
SEE TYPICAL PANEL
INSTALLATION SHEET



STANDARD SIGN
SIGN BANDING DETAILS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer
DATE 6/10/19 PLATE NO. A5-9.4



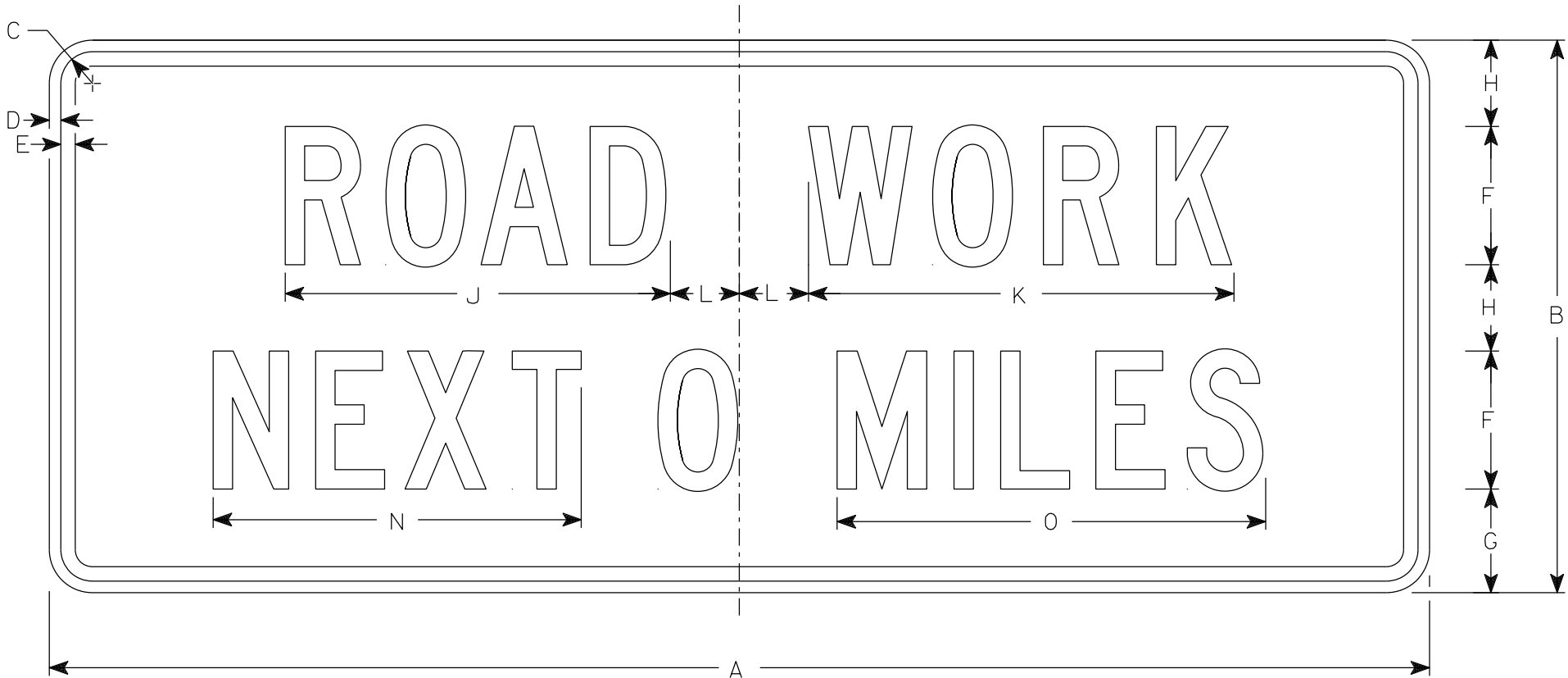
GENERAL NOTES

1. WOOD 4"X6" POST MATERIAL SHALL CONFORM TO 507.2.2 OF THE WISDOT STANDARD SPECIFICATIONS
2. BLOCK BANDING AND CLIPS SHALL BE STAINLESS STEEL, $\frac{3}{4}$ " WIDTH AND 0.025" THICKNESS
3. SIGNS 3' OR GREATER IN HEIGHT SHALL UTILIZE 3 BLOCK BANDS. SIGNS UNDER 3' IN HEIGHT SHALL UTILIZE 2 BLOCK BANDS
4. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA, BUT NORMALLY THERE ARE TWO. FOR SIGNS GREATER THAN 9 S.F. 3 FASTENERS SHALL BE USED.
5. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
 - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
 - b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3
6. ALL BOLTS SHALL HAVE HEXAGONAL HEADS.
7. STEEL WASHERS SHALL BE $\frac{1}{4}$ " O.D. X $\frac{3}{8}$ " I.D. X $\frac{1}{16}$ "
8. NYLON WASHERS SHALL BE $\frac{1}{4}$ " O.D. X $\frac{3}{8}$ " I.D. X .080 FOR TYPE H OR TYPE F FACE SIGN

✱ LAG BOLTS SHALL BE $\frac{3}{8}$ " X $2\frac{1}{2}$ "

BLOCK BANDING DETAIL (V-BLOCK OPTION)	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R. Rauch</i> for State Traffic Engineer
DATE 4/19/2022	PLATE NO. A5-10.3

7



G20-1

NOTES

- 1. Sign is Type II - Type F Reflective
- 2. Color:
 - Background - Orange
 - Message - Black
- 3. Message Series - C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- 5. Round distance to nearest whole Mile and substitute appropriate numerals and optically adjust spacing to achieve proper balance

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	60	24	1 7⁄8	1⁄2	5⁄8	6	4 1⁄2	3 3⁄4		16 3⁄4	18 1⁄2	3		16	18 5⁄8												10.0
2M	60	24	1 7⁄8	1⁄2	5⁄8	6	4 1⁄2	3 3⁄4		16 3⁄4	18 1⁄2	3		16	18 5⁄8												10.0
3	60	24	1 7⁄8	1⁄2	5⁄8	6	4 1⁄2	3 3⁄4		16 3⁄4	18 1⁄2	3		16	18 5⁄8												10.0
4	60	24	1 7⁄8	1⁄2	5⁄8	6	4 1⁄2	3 3⁄4		16 3⁄4	18 1⁄2	3		16	18 5⁄8												10.0
5	60	24	1 7⁄8	1⁄2	5⁄8	6	4 1⁄2	3 3⁄4		16 3⁄4	18 1⁄2	3		16	18 5⁄8												10.0

STANDARD SIGN

G20-1

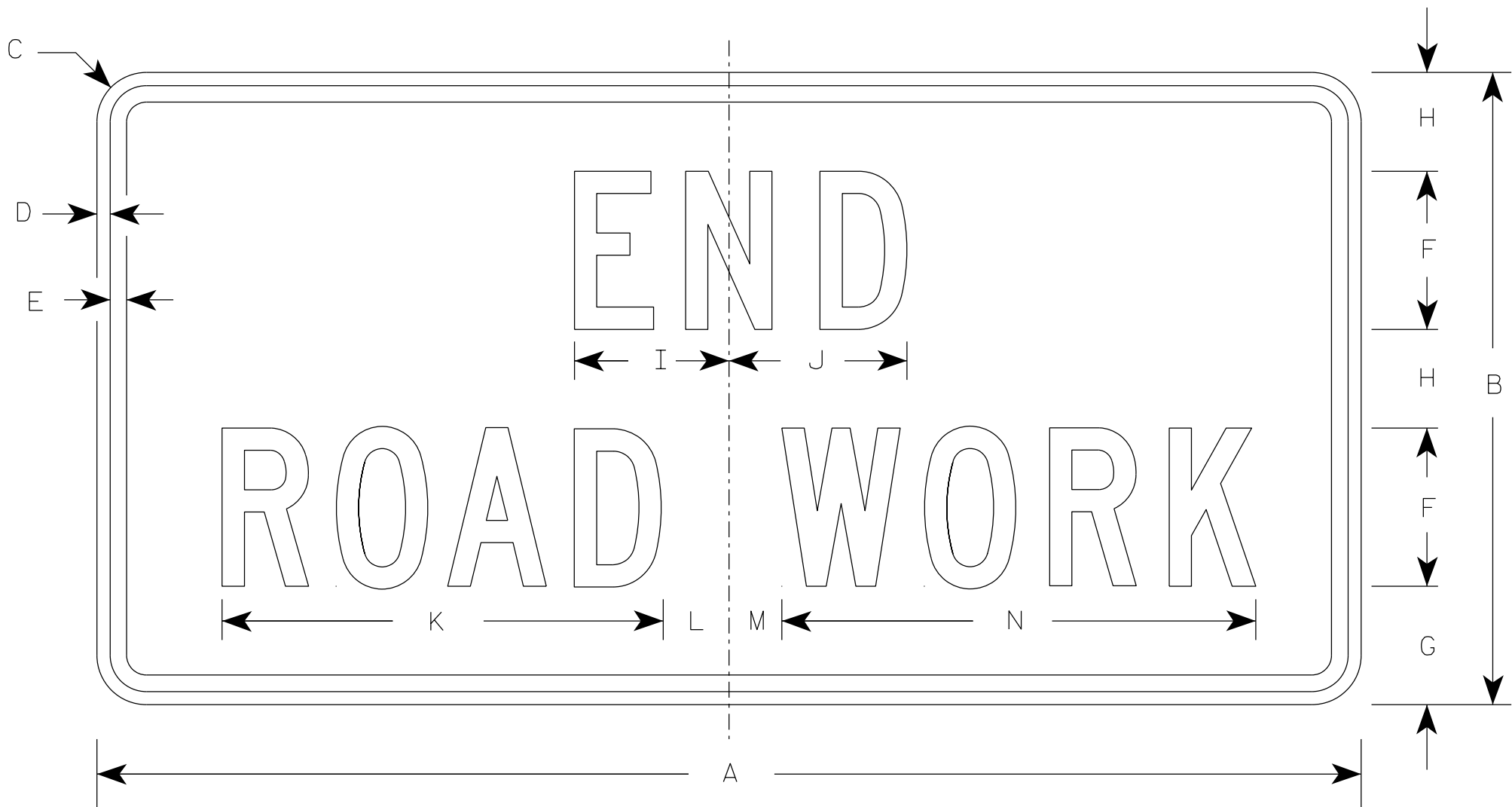
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 1/26/2023 PLATE NO. G20-1.9

NOTES

- 1. Sign is Type II - Type F Reflective
- 2. Color:
Background - Orange
Message - Black
- 3. Message Series - C
- 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



G20-2A

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36	18	1 1/2	3/8	1/2	4	3 3/4	2 1/2	4 1/8	4 1/8	11 1/8	2	1	12 1/8													4.5
2	48	24	1 7/8	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0
2M	48	24	1 7/8	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0
3	48	24	1 7/8	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0
4	48	24	1 7/8	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0
5	48	24	1 7/8	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0

STANDARD SIGN

G20-2A

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 1/26/2023 PLATE NO. G20-2A.10

PROJECT NO:

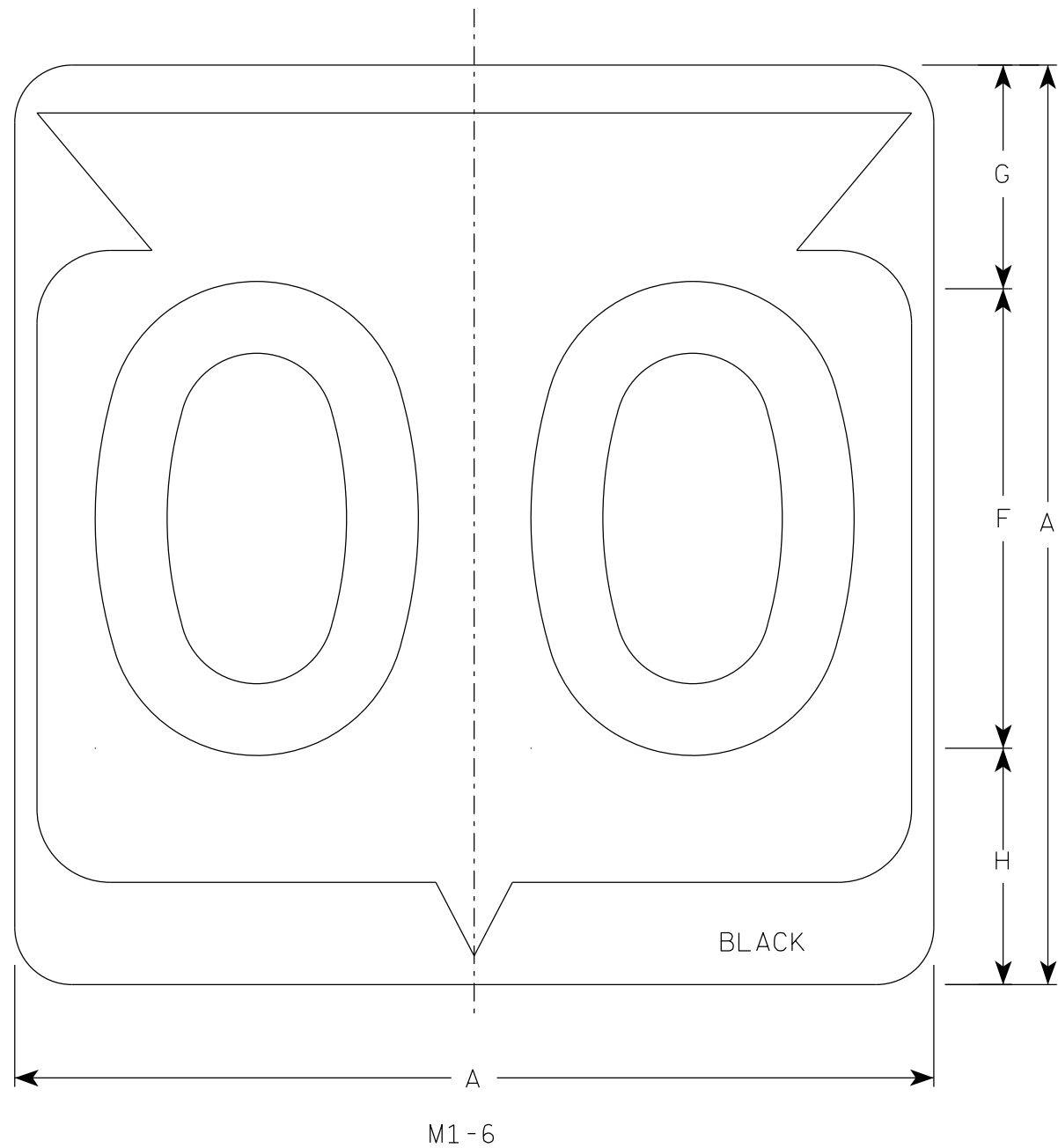
HWY:

COUNTY:

SHEET NO:

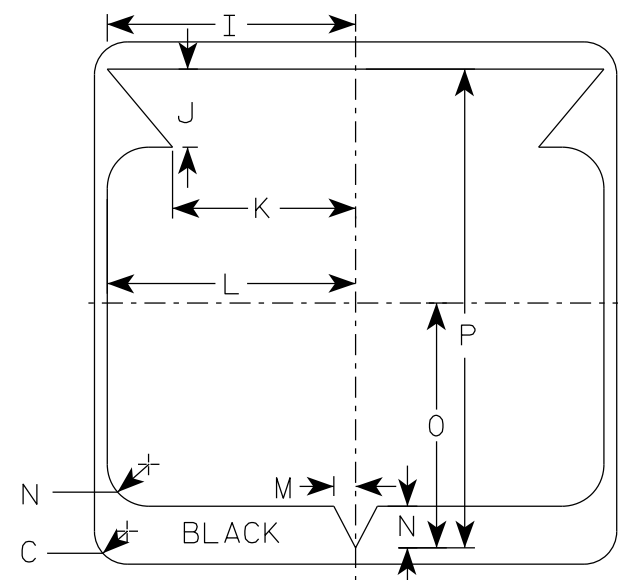
E

7



NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - D except 3 number signs Series C



7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24		1 1/2			12	5 1/2	6 1/2	10 1/4	2 1/2	8 7/8	11 1/2	1	1 7/8	11 1/4	21 7/8											4.0
2M	24		1 1/2			12	5 1/2	6 1/2	10 1/4	2 1/2	8 7/8	11 1/2	1	1 7/8	11 1/4	21 7/8											4.0
3	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0
4	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0
5	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0

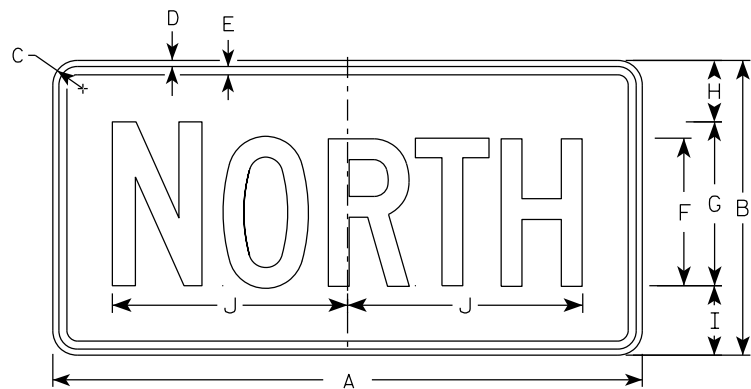
STATE ROUTE MARKER
M1-6 FOR ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

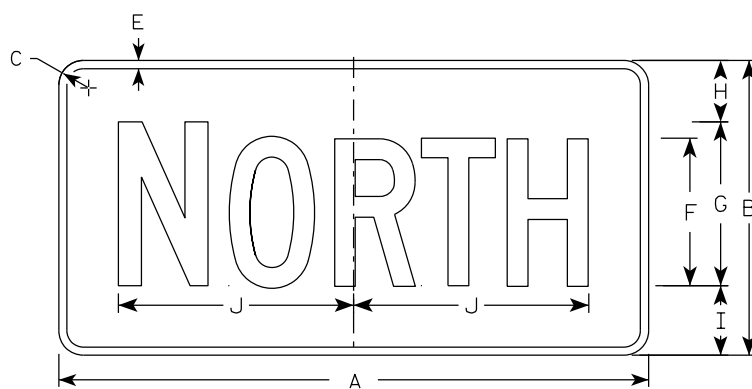
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 11/8/2022 PLATE NO. M1-6.11

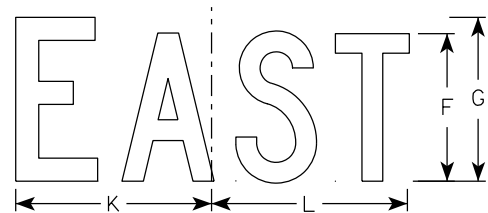
PROJECT NO: HWY: COUNTY: SHEET NO: **E**



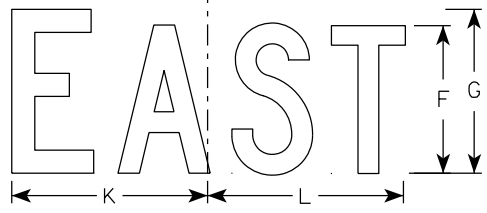
M3-1
MM3-1
MP3-1



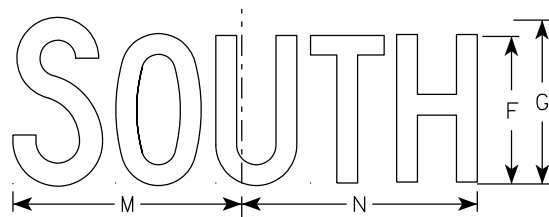
MB3-1
MK3-1
MN3-1



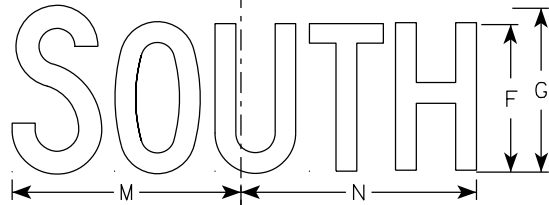
M3-2
MM3-2
MP3-2



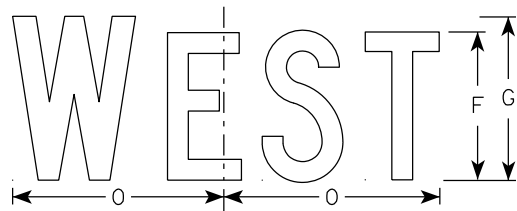
MB3-2
MK3-2
MN3-2



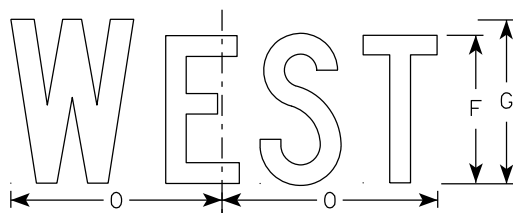
M3-3
MM3-3
MP3-3



MB3-3
MK3-3
MN3-3



M3-4
MM3-4
MP3-4



MB3-4
MK3-4
MN3-4

NOTES

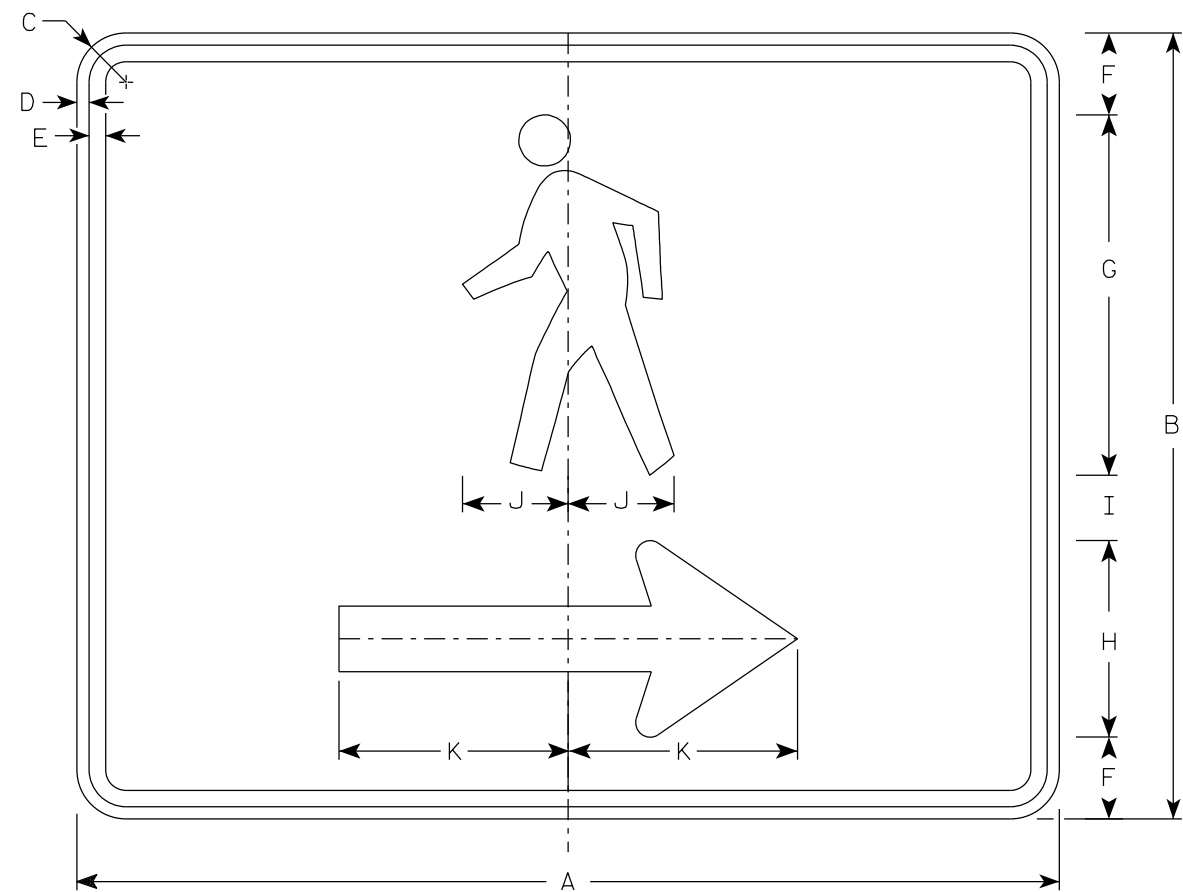
- All Signs Type II - Type H Reflective
- Color:
 - Background - See note 5
 - Message - See note 5
- Message Series - C
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- M3-1 thru M3-4 Background - White
Message - Black
MB3-1 thru MB3-4 Background - Blue
Message - White
MK3-1 thru MK3-4 Background - Green
Message - White
MM3-1 thru MM3-4 Background - White
Message - Green
MN3-1 thru MN3-4 Background - Brown
Message - White
MP3-1 thru MP3-4 Background - White
Message - Blue
- Note the first letter of each direction is larger than the remainder of the message.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	12	1 1/2	3/8	3/8	6	7	2 1/4	2 3/4	10 1/4	7 7/8	8 3/8	10 1/4	9 3/4	8 3/4												2.00
2M	24	12	1 1/2	3/8	3/8	6	7	2 1/4	2 3/4	10 1/4	7 7/8	8 3/8	10 1/4	9 3/4	8 3/4												2.00
3	36	18	1 1/2	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13												4.5
4	36	18	1 1/2	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13												4.5
5	36	18	1 1/2	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13												4.5

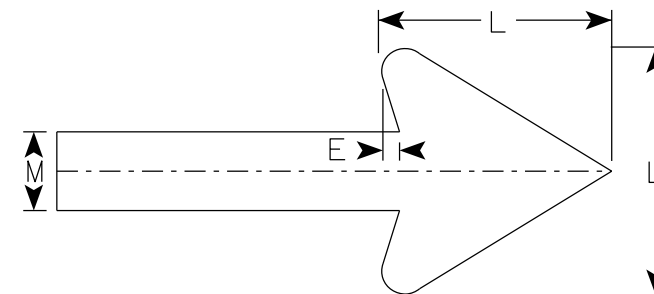
PROJECT NO:	HWY:	COUNTY:	SHEET NO:	E
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M4-60R

NOTES

1. Sign is Type II- Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. M4-60L is the same as M4-60R except the arrow is reversed.

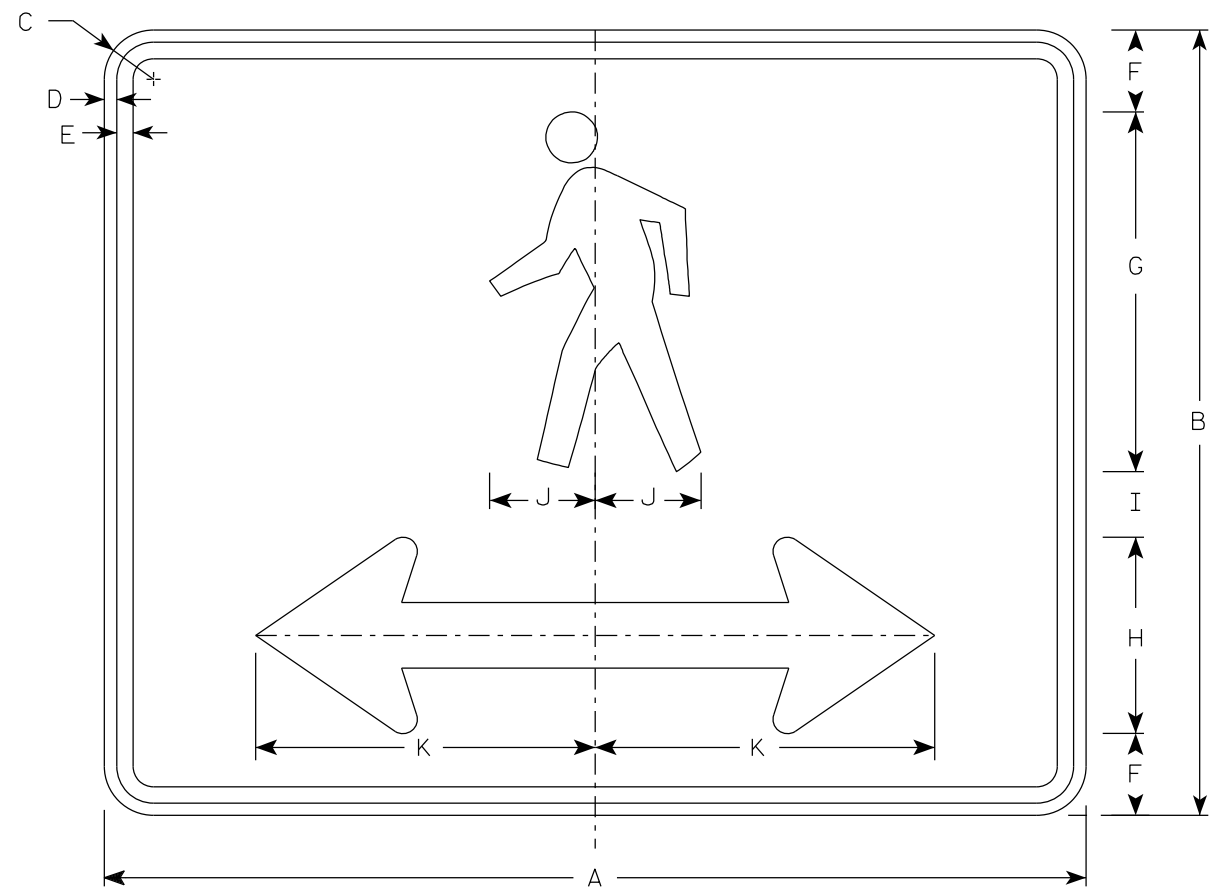


Arrow Detail

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30	24	1 1/2	3/8	1/2	2 1/2	11	6	2	3 1/4	7	6	2														5.00
2M	30	24	1 1/2	3/8	1/2	2 1/2	11	6	2	3 1/4	7	6	2														5.00
3																											
4																											
5																											

PROJECT NO:	HWY:	COUNTY:	SHEET NO:	E
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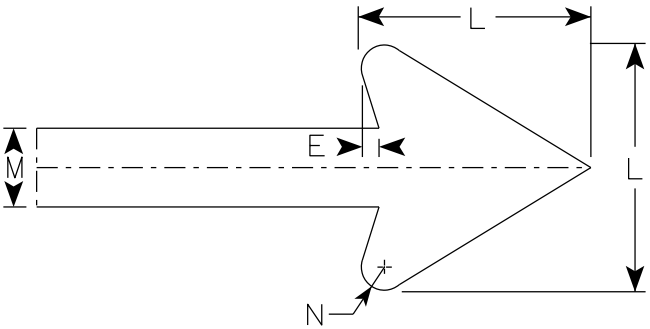
7



M4-60D

NOTES

- 1. Sign is Type II- Type F Reflective
- 2. Color:
Background - Orange
Message - Black
- 3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



Arrow Detail

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30	24	1 1/2	3/8	1/2	2 1/2	11	6	2	3 1/4	10 3/8	6	2	3/8													5.00
2M	30	24	1 1/2	3/8	1/2	2 1/2	11	6	2	3 1/4	10 3/8	6	2	3/8													5.00
3																											
4																											
5																											

PROJECT NO:

HWY:

COUNTY:

SHEET NO:

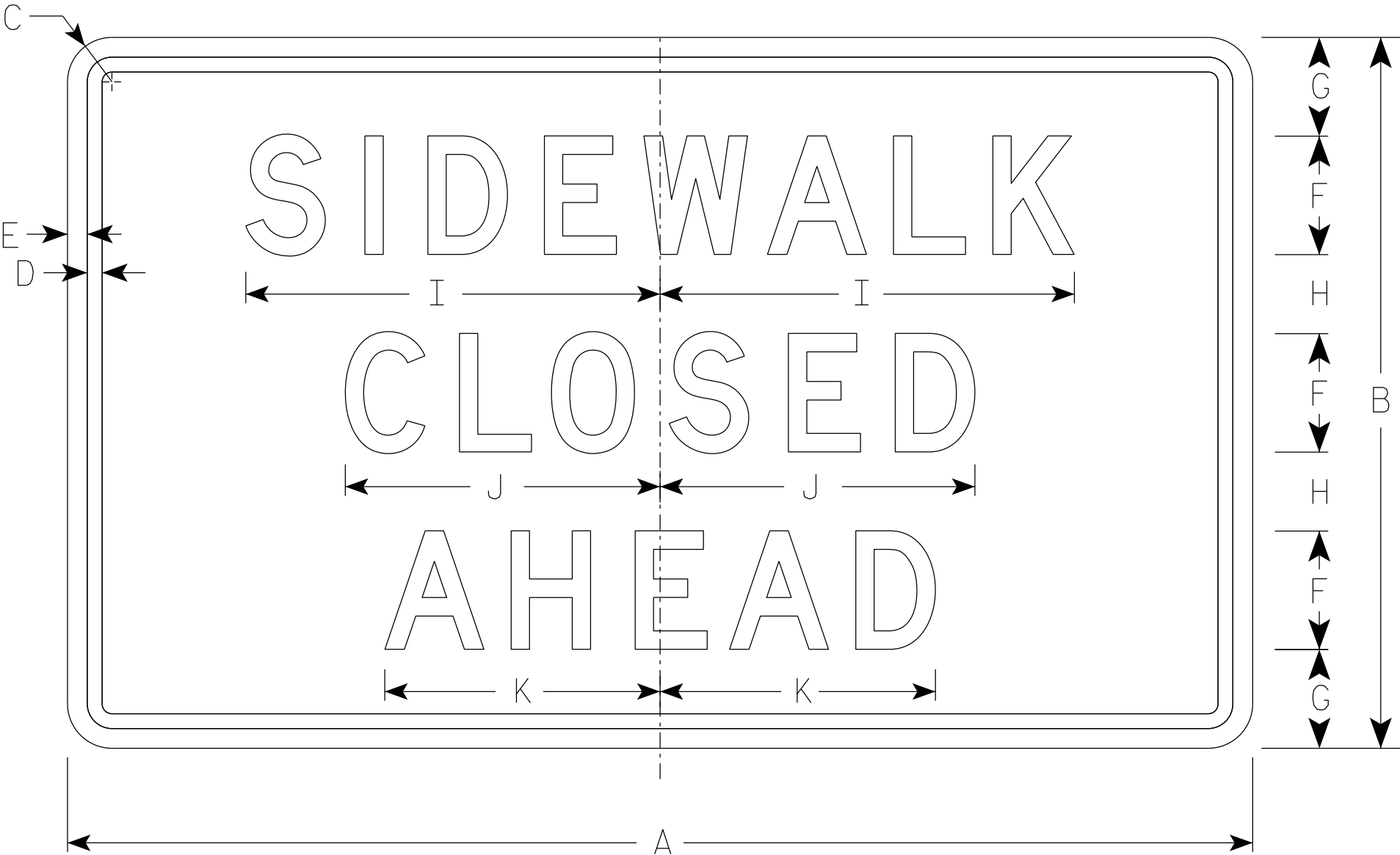
E

NOTES

1. Sign is Type II - Type H Reflective
2. Color:

Background - White

Message - Black
3. Message Series - D



R9-9A

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30	18	1 1/2	3/8	1/2	3	2 1/2	2	10 1/2	8	7																3.75
2M	30	18	1 1/2	3/8	1/2	3	2 1/2	2	10 1/2	8	7																3.75
3																											
4																											
5																											

PROJECT NO:

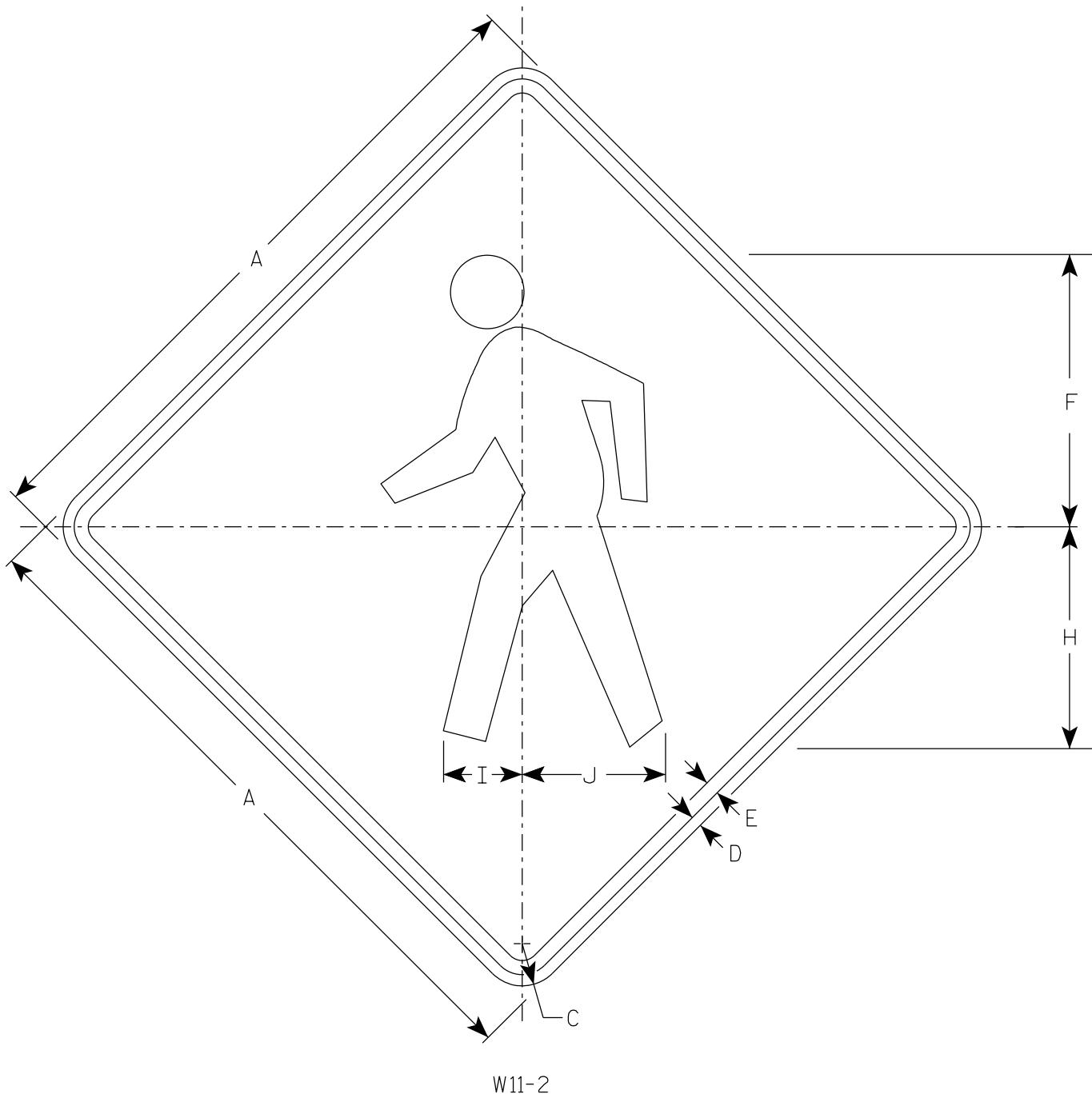
HWY:

COUNTY:

SHEET NO:

E

7



NOTES

- 1. Sign is Type II - Type F Reflective
- 2. Color:
 - Background - Yellow
 - Message - Black

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/2	3/8	1/2	9 3/4		7 7/8	2 7/8	5 1/8																	4.0
2S	30		1 7/8	1/2	5/8	12 1/8		9 7/8	3 1/2	6 3/8																	6.25
2M	36		2 1/4	5/8	3/4	14 1/2		11 7/8	4 1/4	7 5/8																	9.0
3	36		2 1/4	5/8	3/4	14 1/2		11 7/8	4 1/4	7 5/8																	9.0
4	48		3	3/4	1	19 3/8		15 3/4	5 5/8	10 1/4																	16.0
5																											

STANDARD SIGN
W11-2

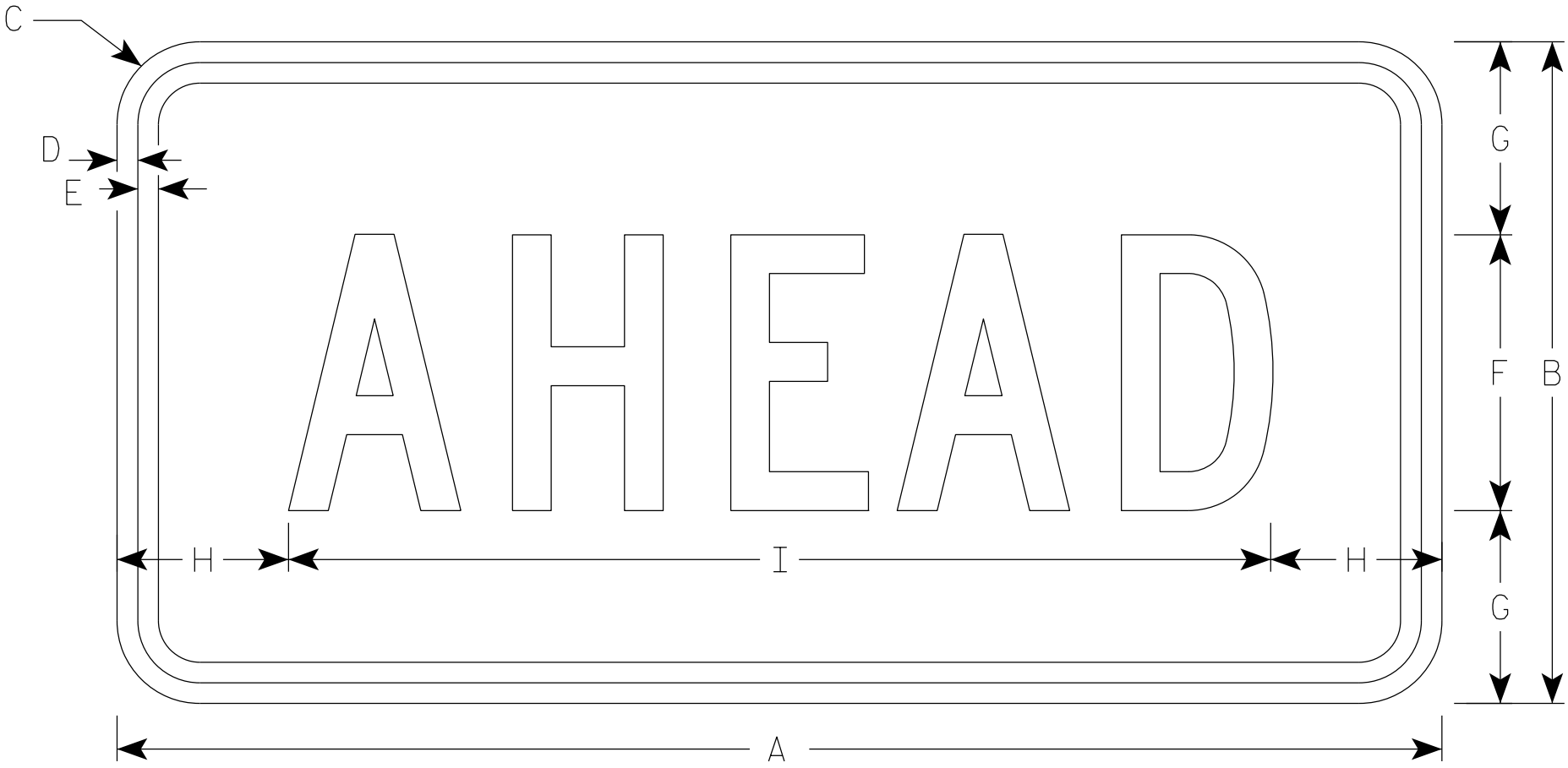
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 6/15/2023 PLATE NO. W11-2.9

NOTES

- 1. Sign is Type II - Type F Reflective
- 2. Color:
Background - Yellow
Message - Black
- 3. Message Series - C



W16-9P

- * For 36" x 36" Warning Signs, use 30" x 18" W16-9P signs.
- * For 48" x 48" Warning Signs, use 48" x 24" W16-9P signs.

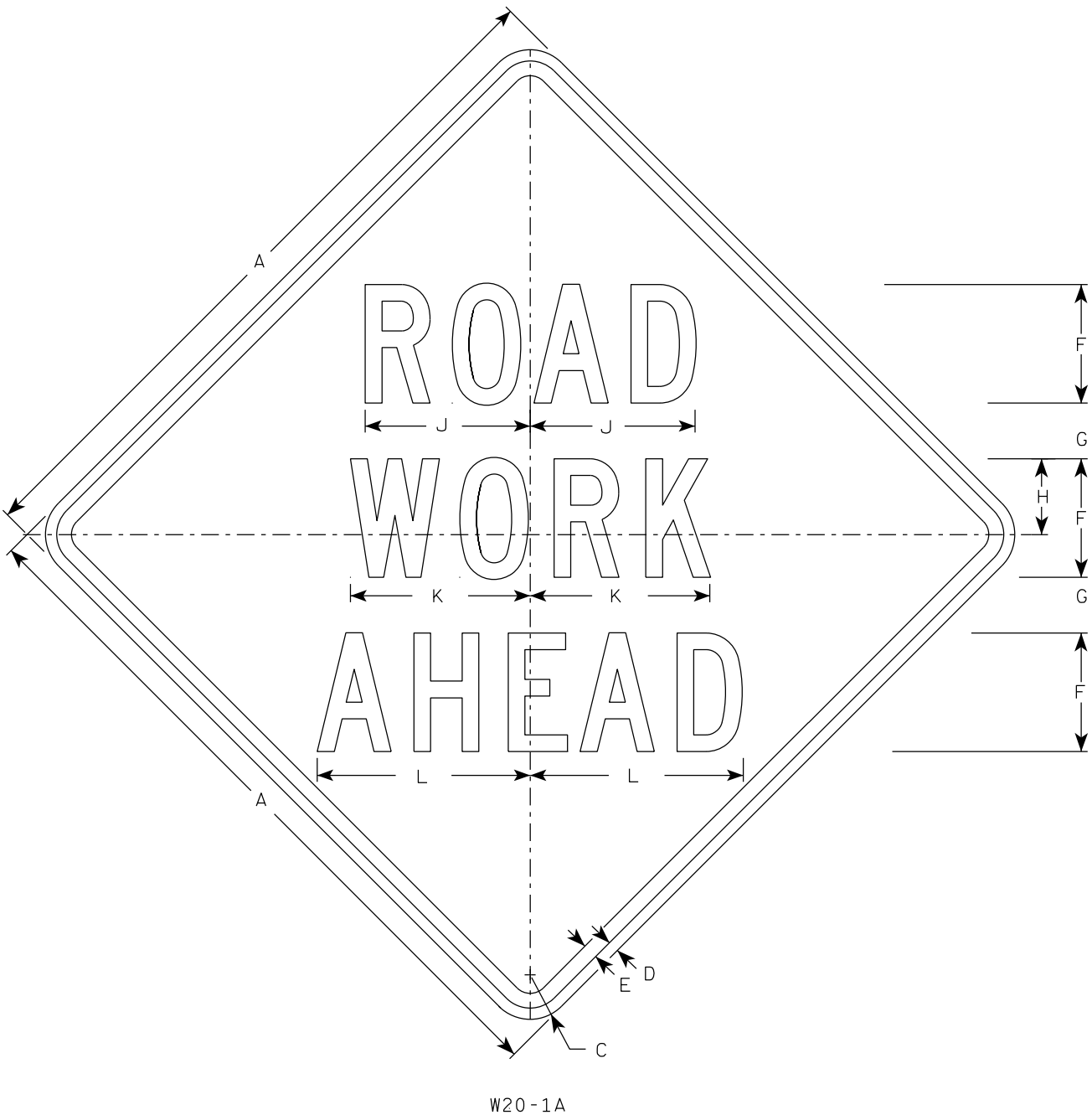
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	12	1 1/2	3/8	3/8	5	3 1/2	3 1/8	17 3/4																		2.0
2M	30	18	1 1/2	3/8	1/2	7	5 1/2	2 3/4	24 1/2																		3.75
3	30	18	1 1/2	3/8	1/2	7	3 1/2	2 3/4	24 1/2																		3.75
4	48	24	1 7/8	1/2	5/8	10	7	6 1/8	35 3/4																		8.0
5																											

STANDARD SIGN
W16-9P

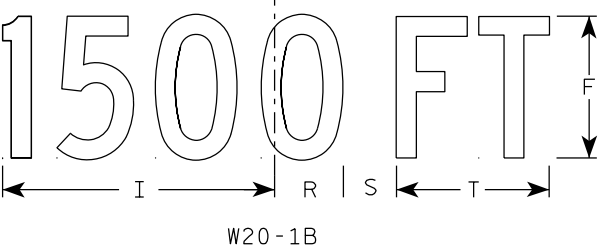
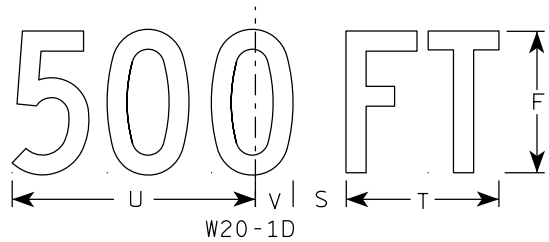
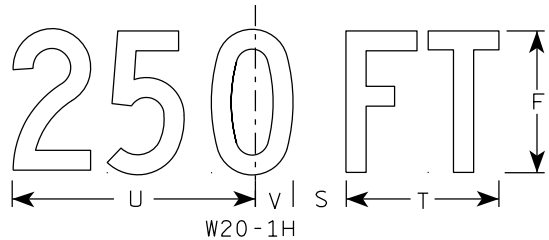
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 1/9/2024 PLATE NO. W16-9P.9

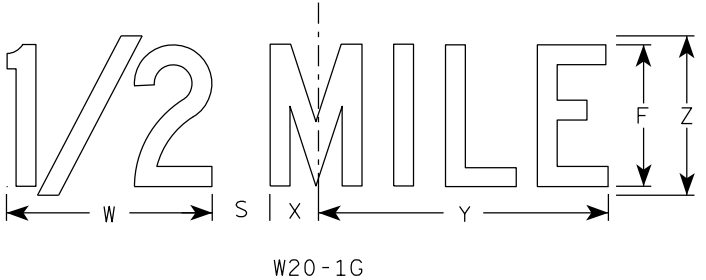


W20-1A

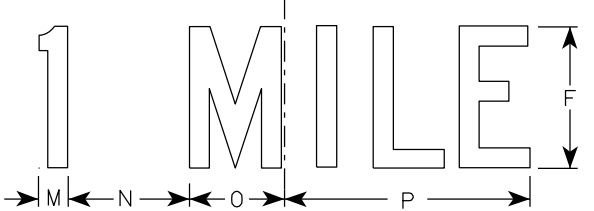


W20-1B

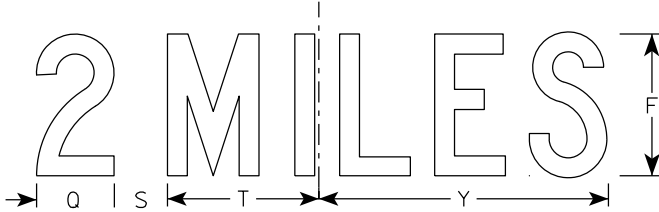
- NOTES
1. Sign is Type II - Type F Reflective
 2. Color:
Background - Orange
Message - Black
 3. Message Series - C
 4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



W20-1G



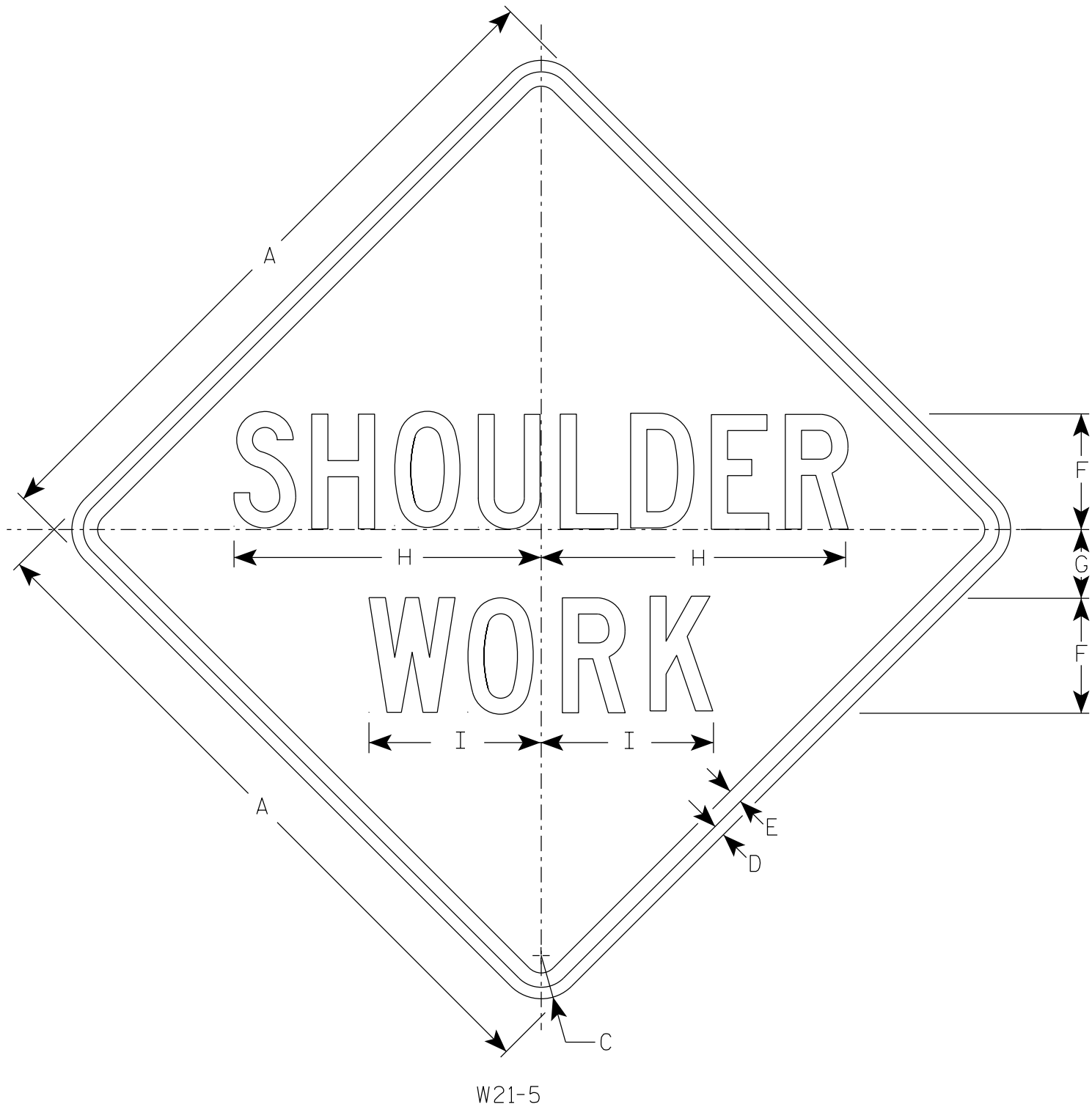
W20-1F



W20-1E

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	v	W	X	Y	Z	Area sq. ft.
1	36		2 1/4	5/8	3/4	5	2 5/8	3 1/4	10 1/8	7	7 5/8	8 7/8	1 1/8	4 1/2	3 1/2	9	3 1/4	2 1/2	2 1/4	5 5/8	9	1 3/8	8	1 3/4	10 3/4	6	9.0
2S	48		3	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
2M	48		3	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
3	48		3	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
4	48		3	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
5	48		3	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0

7



NOTES

- 1. Sign is Type II - Type F Reflective
- 2. Color:
 - Background - Orange
 - Message - Black
- 3. Message Series - C

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		2 1/4	5/8	3/4	6	3 1/2	16	9																		9.0
2S	48		3	3/4	1	8	5	21 3/8	11 1/4																		16.0
2M	48		3	3/4	1	8	5	21 3/8	11 1/4																		16.0
3	48		3	3/4	1	8	5	21 3/8	11 1/4																		16.0
4	48		3	3/4	1	8	5	21 3/8	11 1/4																		16.0
5	48		3	3/4	1	8	5	21 3/8	11 1/4																		16.0

STANDARD SIGN

W21-5

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 1/11/2024 PLATE NO. W21-5.7

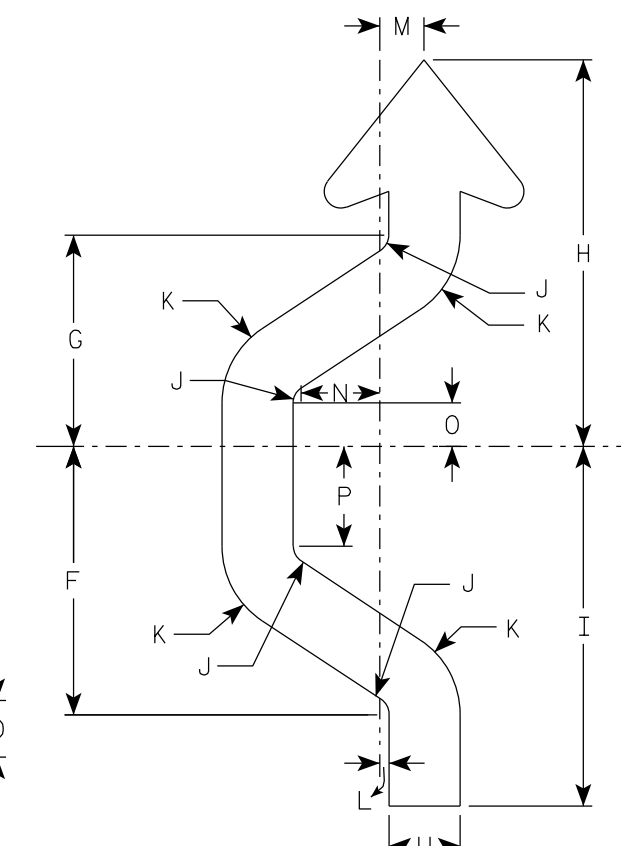
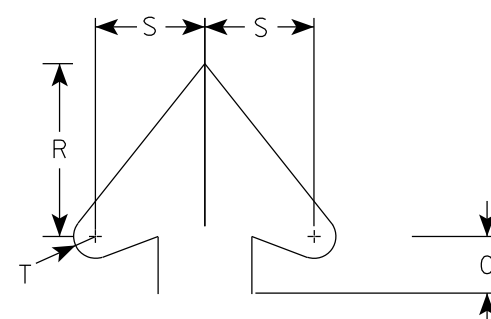
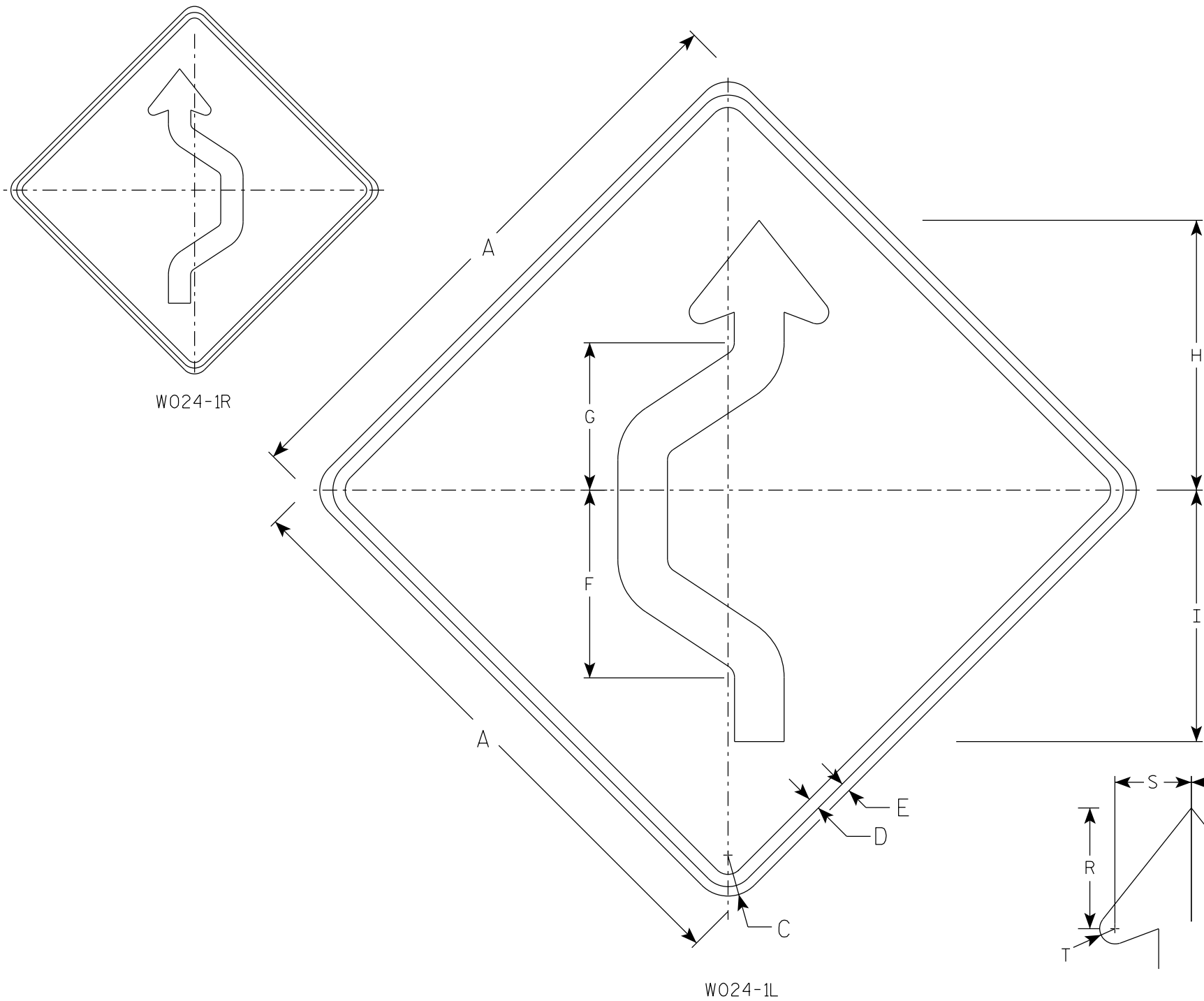
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E



NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. W024-1R is the same as W024-1L except reversed along the vertical centerline.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		2 1/4	5/8	1/2	11 3/8	8 7/8	16 3/8	15 1/4	3/4	3 3/4	3/8	2	3 3/8	1 7/8	4 1/4		5 1/2	3 1/2	5/8	3						9.0
2S	48		3	1	3/4	15 1/8	11 7/8	21 3/4	20 1/4	1	5	1/2	2 1/2	4 3/8	2 3/8	5 5/8		7 3/8	4 3/4	7/8	4						16.0
2M	48		3	1	3/4	15 1/8	11 7/8	21 3/4	20 1/4	1	5	1/2	2 1/2	4 3/8	2 3/8	5 5/8		7 3/8	4 3/4	7/8	4						16.0
3	48		3	1	3/4	15 1/8	11 7/8	21 3/4	20 1/4	1	5	1/2	2 1/2	4 3/8	2 3/8	5 5/8		7 3/8	4 3/4	7/8	4						16.0
4	48		3	1	3/4	15 1/8	11 7/8	21 3/4	20 1/4	1	5	1/2	2 1/2	4 3/8	2 3/8	5 5/8		7 3/8	4 3/4	7/8	4						16.0
5	48		3	1	3/4	15 1/8	11 7/8	21 3/4	20 1/4	1	5	1/2	2 1/2	4 3/8	2 3/8	5 5/8		7 3/8	4 3/4	7/8	4						16.0

STANDARD SIGN

W024-1 L & R

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 2/1/2024 PLATE NO. W024-1.2

PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E

Notes



Wisconsin Department of Transportation

Dedicated people creating transportation solutions
through innovation and exceptional service.

<http://www.dot.wisconsin.gov>