RHI

MARCH 2026

ORDER OF SHEETS

Section No.	1	Title
Section No.	2	Typical Sections and Details
Section No.	3	Estimate of Quantities
Section No.	3	Miscellaneous Quantities
~~Section No.~~	~~4~~	~~Right of Way Plat~~
Section No.	5	Plan and Profile
Section No.	6	Standard Detail Drawings
Section No.	7	Sign Plates
Section No.	8	Structure Plans
Section No.	9	Computer Earthwork Data
Section No.	9	Cross Sections

TOTAL SHEETS = 46

PROJECT ID: 8724-04-71

WITH: N/A

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

T OGEMA, LARKIN ROAD

HOLMES CREEK BRIDGE, B-50-0097

LOC STR
PRICE COUNTY

STATE PROJECT NUMBER
8724-04-71

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
8724-04-71		

ACCEPTED FOR

TOWN OF OGEMA

10/13/2025 Highway Commissioner

Date (Signature and Title of Official)

ORIGINAL PLANS PREPARED BY

WESTBROOK
Associated Engineers, Inc.
619 EAST HOXIE STREET
P.O. BOX 429
SPRING GREEN, WISCONSIN 53588
PHONE (608) 588-7866
FAX (608) 588-7954

WISCONSIN
AARON B. PALMER
E-35695
RICHLAND CENTER, WI
PROFESSIONAL ENGINEER

DATE: Oct. 13, 25 (Professional Engineer Signature)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY

Surveyor	WESTBROOK ASSOCIATED ENGINEERS, INC.
Designer	WESTBROOK ASSOCIATED ENGINEERS, INC.
Project Manager	MICHAEL GRAGE, P.E.
Regional Examiner	NORTH CENTRAL REGION
Regional Supervisor	DAN ERVA, P.E.

APPROVED FOR THE DEPARTMENT

DATE: 10/15/2025 (Signature)

E

N

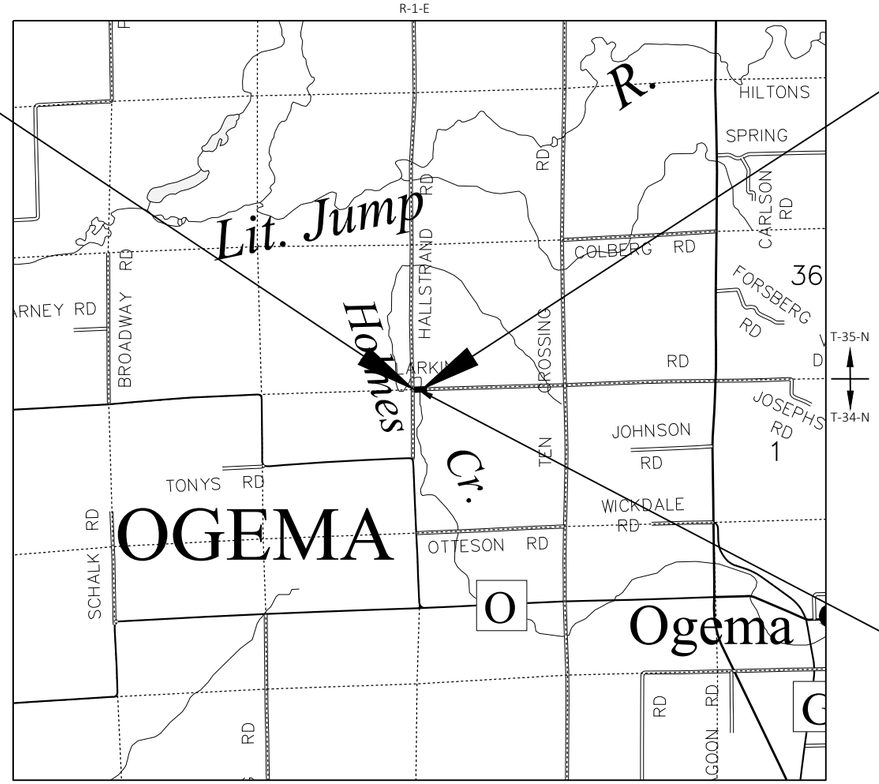
DESIGN DESIGNATION 8724-04-01

A.A.D.T.	2026	=	58
A.A.D.T.	2046	=	65
D.H.V.		=	10
D.D.		=	62 / 38
T.		=	7.7%
DESIGN SPEED		=	60 MPH
ESALS		=	11,300

BEGIN PROJECT
STA 11+83.73
Y: 331 880.009
X: 783 365.248

END PROJECT
STA 13+16.27

STRUCTURE B-50-0097
STA 12+50.00



LAYOUT

SCALE 0 1 MI

TOTAL NET LENGTH OF CENTERLINE = 0.025 MILES

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS), PRICE COUNTY NAD83 (2011), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES.

ELEVATIONS ARE REFERENCED TO NAVD 88 (2012). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 18

CONVENTIONAL SYMBOLS

PLAN

- CORPORATE LIMITS
- PROPERTY LINE
- LOT LINE
- LIMITED HIGHWAY EASEMENT
- EXISTING RIGHT OF WAY
- PROPOSED OR NEW R/W LINE
- SLOPE INTERCEPT
- REFERENCE LINE (300'EB')
- EXISTING CULVERT
- PROPOSED CULVERT (Box or Pipe)
- COMBUSTIBLE FLUIDS (CAUTION)
- MARSH AREA
- WOODED OR SHRUB AREA

PROFILE

- GRADE LINE
- ORIGINAL GROUND
- MARSH OR ROCK PROFILE (To be noted as such) (ROCK)
- SPECIAL DITCH (LABEL)
- GRADE ELEVATION (95.36)
- CULVERT (Profile View)

UTILITIES

- ELECTRIC (E)
- FIBER OPTIC (FO)
- GAS (G)
- SANITARY SEWER (SAN)
- STORM SEWER (SS)
- TELEPHONE (T)
- WATER (W)
- UTILITY PEDESTAL
- POWER POLE
- TELEPHONE POLE

COUNTY: PRICE

30

STANDARD ABBREVIATIONS

ABUT	ABUTMENT	MGAL	ONE THOUSAND GALLONS
AC	ACRE	ML OR M/L	MATCH LINE
AGG	AGGREGATE	NOM	NOMINAL
AH	AHEAD	NC	NORMAL CROWN
	ANGLE	NB	NORTHBOUND
AADT	ANNUAL AVERAGE DAILY TRAFFIC	NO	NUMBER
ASPH	ASPHALTIC	OD	OUTSIDE DIAMETER
BK	BACK	PAVT	PAVEMENT
BAD	BASE AGGREGATE DENSE	PC	POINT OF CURVATURE
BL OR B/L	BASE LINE	PI	POINT OF INTERSECTION
BM	BENCH MARK	PT	POINT OF TANGENCY
CL OR C/L	CENTER LINE	PCC	PORTLAND CEMENT CONCRETE
Δ	CENTRAL ANGLE OR DELTA	LB	POUND
CONC	CONCRETE	PSI	POUNDS PER SQUARE INCH
CONST	CONSTRUCTION	PE	PRIVATE ENTRANCE
CP	CONTROL POINT	PROJ	PROJECT
CO	COUNTY	PL	PROPERTY LINE
CTH	COUNTY TRUCK HIGHWAY	R	RADIUS
CY	CUBIC YARD	RL OR R/L	REFERENCE LINE
D	DEGREE OF CURVE	REQD	REQUIRED
DHV	DESIGN HOUR VOLUME	RT	RIGHT
DIA	DIAMETER	RHF	RIGHT HAND FORWARD
DD	DIRECTIONAL DISTRIBUTION	R/W	RIGHT OF WAY
DWY	DRIVEWAY	RD	ROAD
EA	EACH	RDWY	ROADWAY
EB	EASTBOUND	SHLDR	SHOULDER
EL OR ELEV	ELEVATION	SW	SIDEWALK
EMB	EMBANKMENT	SB	SOUTHBOUND
ESALS	EQUIVALENT SINGLE AXLE LOADS	SPECS	SPECIFICATIONS
EXC	EXCAVATION	SF	SQUARE FEET
EXIST	EXISTING	SY	SQUARE YARD
FERT	FERTILIZER	SDD	STANDARD DETAIL DRAWINGS
FL OR F/L	FLOW LINE	STH	STATE TRUNK HIGHWAY
FT	FOOT	STA	STATION
HES	HIGH EARLY STRENGTH	SE	SUPERELEVATION
CWT	HUNDRED WEIGHT	SL OR S/L	SURVEY LINE
IN DIA	INCH DIAMETER	TEMP	TEMPORARY
ID	INSIDE DIAMETER	T	TRUCKS (PERCENT OF)
IH	INTERSTATE HIGHWAY	TYP	TYPICAL
INV	INVERT	USH	UNITED STATES HIGHWAY
JT	JOINT	VAR	VARIABLE
LT	LEFT	VC	VERTICAL CURVE
LHF	LEFT HAND FORWARD	VPC	VERTICAL POINT OF CURVATURE
L	LENGTH OF CURVE	VPI	VERTICAL POINT OF INTERSECTION
LF	LINEAR FOOT	VPT	VERTICAL POINT OF TANGENCY
LC	LONG CHORD OF CURVE	W	WEST
LS	LUMP SUM	WB	WESTBOUND

WISCONSIN DNR LIAISON

WENDY HENNIGES
NORTH CENTRAL REGION
107 SUTLIFF AVE
RHINELANDER, WI 54501
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EMAIL: Wendy.Henniges@wisconsin.gov

DESIGN PROJECT MANAGER

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EMAIL: apalmer@westbrookeng.com

COUNTY HIGHWAY COMMISSIONER

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PRICE COUNTY
704 N LAKE AVE
PHILLIPS, WI 54555
PHONE: (715) 339-3081
EMAIL: roger.petrick@pricecounty.wi.gov

UTILITIES CONTACTS

BRIGHTSPEED
COMMUNICATIONS
SHAUN MOLLOY
1600 PEARL ST SUITE 300
BOULDER, CO 80302
PHONE: 815-761-8820
EMAIL: smolloy@congruex.com

PRICE ELECTRIC COOPERATIVE
ELECTRICAL
BEN ORYSEN
W6803 SPRINGS DRIVE
PHILLIPS, WI 54555
PHONE: 715-360-2264
EMAIL: borysen@PriceElectric.coop

GENERAL NOTES

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR RESHAPING AND SEEDING ANY PREVIOUSLY GRASSED AREAS WHICH ARE DISTURBED BY OPERATIONS, OUTSIDE OF THE NORMAL CONSTRUCTION LIMITS.

HMA PAVEMENT WEIGHT CALCULATIONS ARE BASED ON 112 LBS/SY/IN.

APPLY TACK COAT BETWEEN LAYERS OF HMA PAVEMENT AT A RATE OF 0.05 GAL/SY.

NO TREES OR SHRUBS ARE TO BE REMOVED WITHOUT THE APPROVAL OF THE ENGINEER.

RIGHT OF WAY INFORMATION SHOWN ON THE PLANS IS APPROXIMATE.

THE CONTRACTOR IS TO WORK WITH UTMOST CARE AND PROTECT ALL SURVEY MARKERS. REMOVAL OF ANY SURVEY MARKER IS TO BE WITH THE APPROVAL OF THE ENGINEER.

THE CONTRACTOR SHALL PREPARE AN EROSION CONTROL IMPLEMENTATION PLAN (ECIP) AND SUBMIT THE PLAN TO WISDOT AND WDNR FOR REVIEW AT LEAST 14 DAYS PRIOR TO THE PRECONSTRUCTION CONFERENCE.

WHEN THE QUANTITY OF THE ITEMS OF BASE AGGREGATE, SUBBASE OR HMA PAVEMENT IS MEASURED FOR PAYMENT BY THE TON OR CUBIC YARD, THE DEPTH OR THICKNESS OF THE LAYERS SHOWN ON THE PLAN IS APPROXIMATE AND THE ACTUAL THICKNESS WILL DEPEND ON THE DISTRIBUTION OF THE MATERIAL AS DIRECTED BY THE ENGINEER.

EROSION CONTROL FEATURES AS SHOWN IN THE PLANS ARE AT APPROXIMATE LOCATIONS. EXACT LOCATIONS WILL BE DETERMINED BY THE CONTRACTOR'S ECIP AND APPROVED BY THE ENGINEER. MAINTAIN EROSION CONTROL MEASURES UNTIL SUCH A TIME AS THE ENGINEER DETERMINES THE MEASURE IS NO LONGER NECESSARY.

BIODEGRADABLE NON-NETTED MATTING SHALL BE USED ALONG STREAM CORRIDORS.

APPLY SEED, EROSION MAT, AND FERTILIZER TO ALL DISTRIBUTED AREAS WITHIN 7 WORKING DAYS AFTER GRADING WORK IS COMPLETE.

THE PROPOSED SHOULDER WIDTH SHOWN IN THE TYPICAL SECTIONS ARE MINIMUM WIDTH. PERPETUATE EXISTING SHOULDERS THAT ARE WIDER THAN WHAT IS SHOWN IN THE TYPICAL SECTIONS.

THE 4-INCH ASPHALTIC SURFACE SHALL BE CONSTRUCTED USING ONE (1) 1.75-INCH LAYER AND ONE (1) 2.25-INCH LAYER. THE LOWER LAYER SHALL BE 2.25-INCHES OF 3 LT 58-28 S. THE UPPER LAYER SHALL BE 1.75-INCHES OF 4 LT 58-28 S.

THE CONTRACTOR'S PAVING OPERATION SHALL BE CONSISTENT WITH THE TYPICAL SECTIONS AND CONSTRUCTED TO PREVENT LONGITUDINAL JOINTS FROM BEING LOCATED WITHIN A DRIVING LANE.

SAWCUTS, AS SHOWN ON THE PLANS, ARE SUGGESTED LOCATIONS AND MAY BE ADJUSTED AT THE DISCRETION OF THE ENGINEER TO BETTER SUIT FIELD CONDITIONS.

THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL DRIVEWAYS AT ALL TIMES EXCEPT WHEN PAVING OR PIPE LAYING OPERATIONS REQUIRE THE DRIVEWAY TO BE CLOSED. ACCESS TO DRIVEWAYS SHALL BE RE-ESTABLISHED IMMEDIATELY AFTER OPERATIONS ARE COMPLETED. ACCESS SHALL BE PROVIDED DURING ALL NON-WORKING HOURS.

TRAFFIC CONTROL DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

BEARINGS SHOWN ON THE PLAN ARE TRUE BEARINGS.

DO NOT DRIVE OR STORE EQUIPMENT, OR STORE CONSTRUCTION MATERIALS IN ENVIRONMENTALLY SENSITIVE AREAS, WETLANDS OR WATERWAYS.

RUNOFF COEFFICIENT TABLE

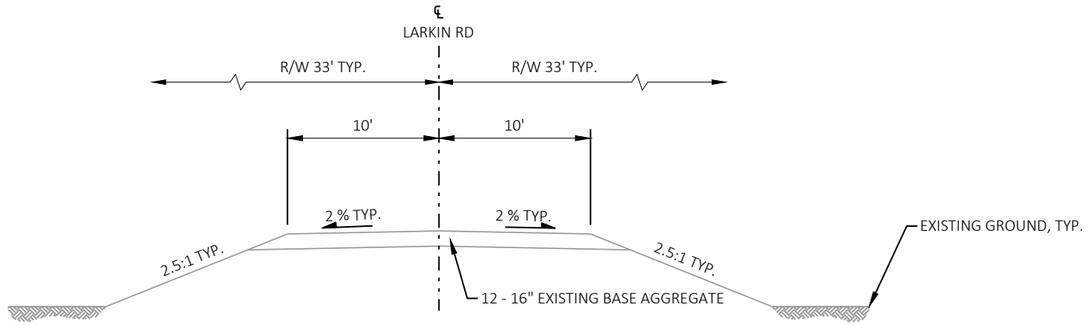
	HYDROLOGIC SOIL GROUP											
	A			B			C			D		
	SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)		
LAND USE:	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER
ROW CROPS:	.08 .22	.16 .30	.22 .38	.12 .26	.20 .34	.27 .44	.15 .30	.24 .37	.33 .50	.19 .34	.28 .41	.38 .56
MEDIAN STRIPTURF:	.19 .24	.20 .26	.24 .30	.19 .25	.22 .28	.26 .33	.20 .26	.23 .30	.30 .37	.20 .27	.25 .32	.30 .40
SIDE SLOPETURF:			.25 .32			.27 .34			.28 .36			.30 .38
PAVEMENT:												
ASPHALT:	.70 - .95											
CONCRETE:	.80 - .95											
BRICK:	.70 - .80											
DRIVES, WALKS:	.75 - .85											
ROOFS:	.75 - .95											
GRAVEL ROADS, SHOULDERS:	.40 - .60											

TOTAL PROJECT AREA = 0.201 ACRES
TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = 0.186 ACRES

DIGGERS HOTLINE
Dial 811 or (800)242-8511
www.DiggersHotline.com

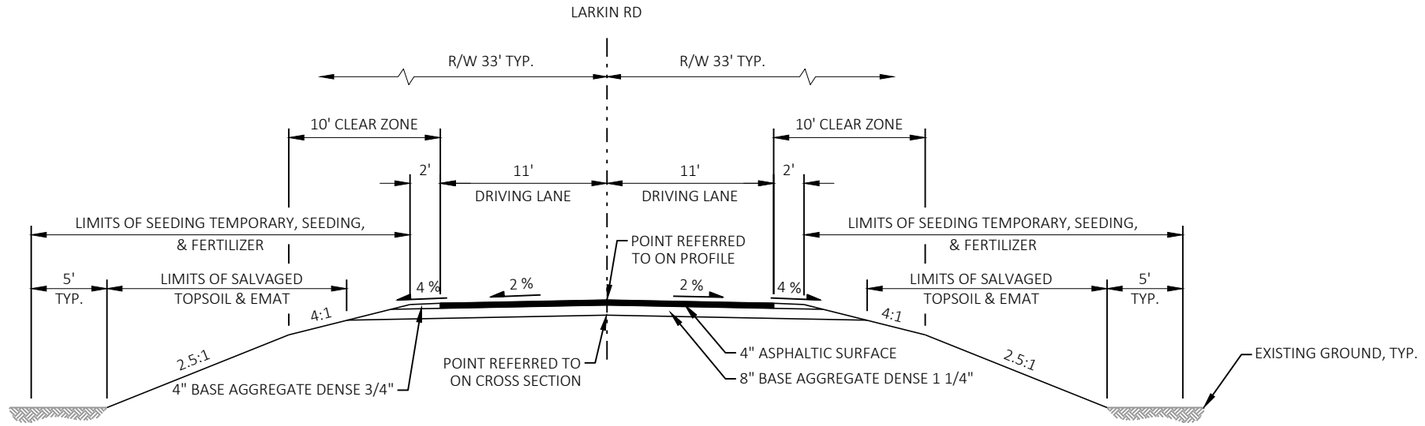
ORDER OF SECTION 2 DETAIL SHEETS

2



EXISTING TYPICAL SECTION

STA 11+83.73 - 13+16.27



FINISHED TYPICAL SECTION

STA 11+83.73 - 13+16.27

PROJECT NO: 8724-04-71	HWY: LARKIN ROAD	COUNTY: PRICE	TYPICAL SECTIONS	SHEET 3	E

LEGEND

- SLOPE INTERCEPT
- SURFACE WATER FLOW
- STREAM EDGE
- SILT FENCE
- WETLANDS
- RIPRAP
- TURBIDITY BARRIER
- EROSION CONTROL BALE

STAGE 1 REMOVAL

- INSTALL TURBIDITY BARRIER TO ENDS OF EAST CULVERT.
- INSTALL WEST SILT FENCE AND TIE INTO TURBIDITY BARRIER
- INSTALL EAST SILT FENCE TO TOP OF SLOPE
- REMOVE WEST AND MIDDLE CULVERTS.
- EXCAVATE AND PERFORM ABUTMENT AND RIPRAP WORK NECESSARY TO PREVENT IN-STREAM DISTURBANCES.
- AFTER RIPRAP IS PLACED, ADJUST WEST SILT FENCE TO TIE INTO RIPRAP.
- REMOVE TURBIDITY BARRIER.

N

STREAM EDGE, TYP.

SLOPE INTERCEPT, TYP.

HOLMES CREEK

EXISTING R/W

FO T

OMP

PT: 11+77.76

REMOVE EXISTING WEST & MIDDLE CULVERTS

PI: 13+55.53

11+00

12+00

13+00

14+00

LARKIN ROAD (AGGREGATE)

SECTION LINE

PROPOSED ABUTMENT

PROPOSED RIPRAP

TURBIDITY BARRIER TYP.

SILT FENCE, TYP.

EXISTING R/W

BEGIN PROJECT
STA 11+83.73

END PROJECT
STA 13+16.27

E

PROJECT NO: 8724-04-71	HWY: LOC STR	COUNTY: PRICE	REMOVAL DETAIL: STAGE 1	SHEET 4	E

FILE NAME : G:\00-PROJECT FILES\2024\24195 ID 8724-04-01, T OGEMA, LARKIN RD, PRICE CO\0-CAD\SHEETS\021101_RM.DWG
LAYOUT NAME - 021101_rm
PLOT DATE : 10/9/2025 3:56 PM PLOT BY : KYLE MAYBERRY PLOT NAME : PLOT SCALE : 1 IN:20 FT
WISDOT/CADDS SHEET 42

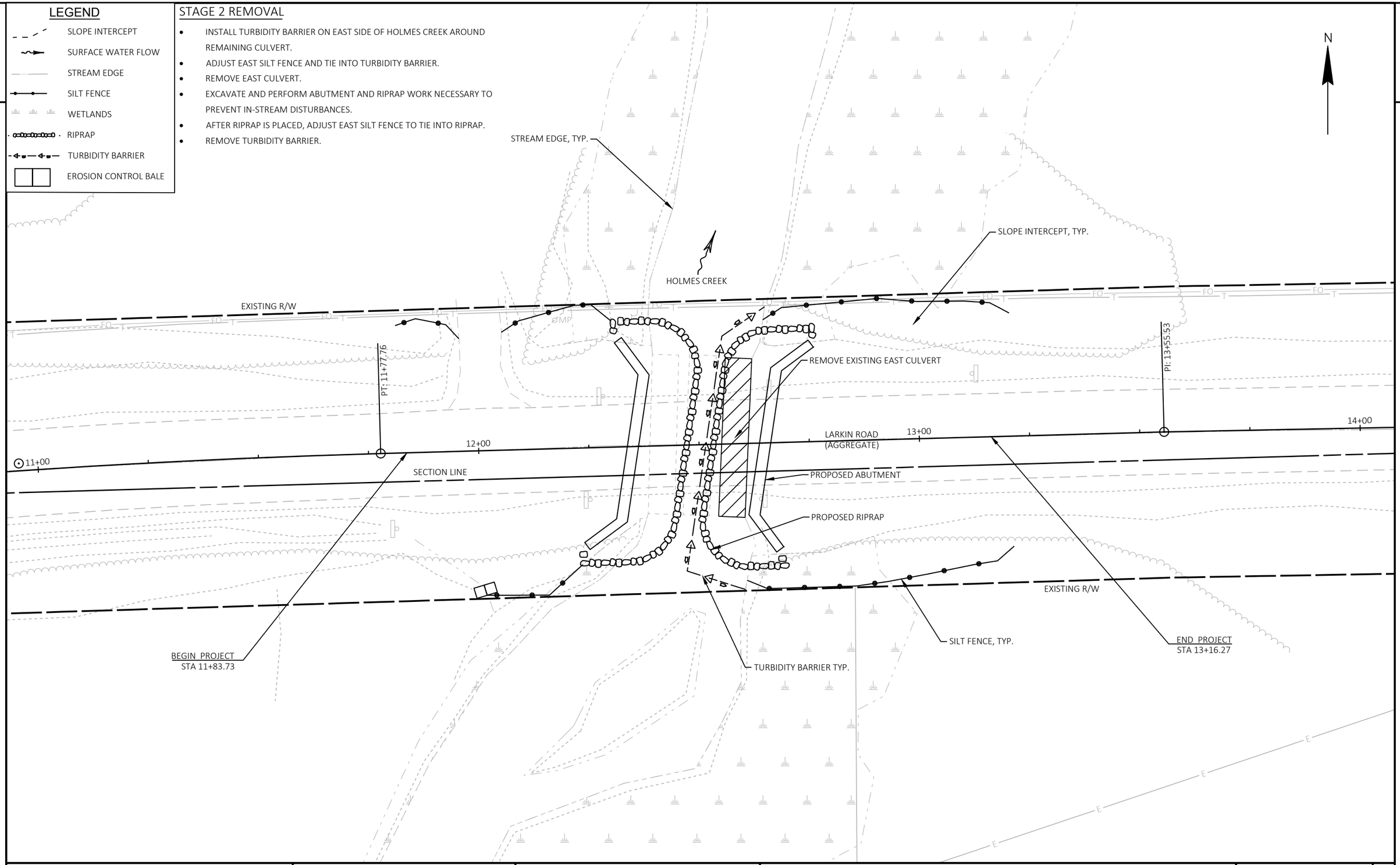
2

LEGEND

- SLOPE INTERCEPT
- SURFACE WATER FLOW
- STREAM EDGE
- SILT FENCE
- WETLANDS
- RIPRAP
- TURBIDITY BARRIER
- EROSION CONTROL BALE

STAGE 2 REMOVAL

- INSTALL TURBIDITY BARRIER ON EAST SIDE OF HOLMES CREEK AROUND REMAINING CULVERT.
- ADJUST EAST SILT FENCE AND TIE INTO TURBIDITY BARRIER.
- REMOVE EAST CULVERT.
- EXCAVATE AND PERFORM ABUTMENT AND RIPRAP WORK NECESSARY TO PREVENT IN-STREAM DISTURBANCES.
- AFTER RIPRAP IS PLACED, ADJUST EAST SILT FENCE TO TIE INTO RIPRAP.
- REMOVE TURBIDITY BARRIER.

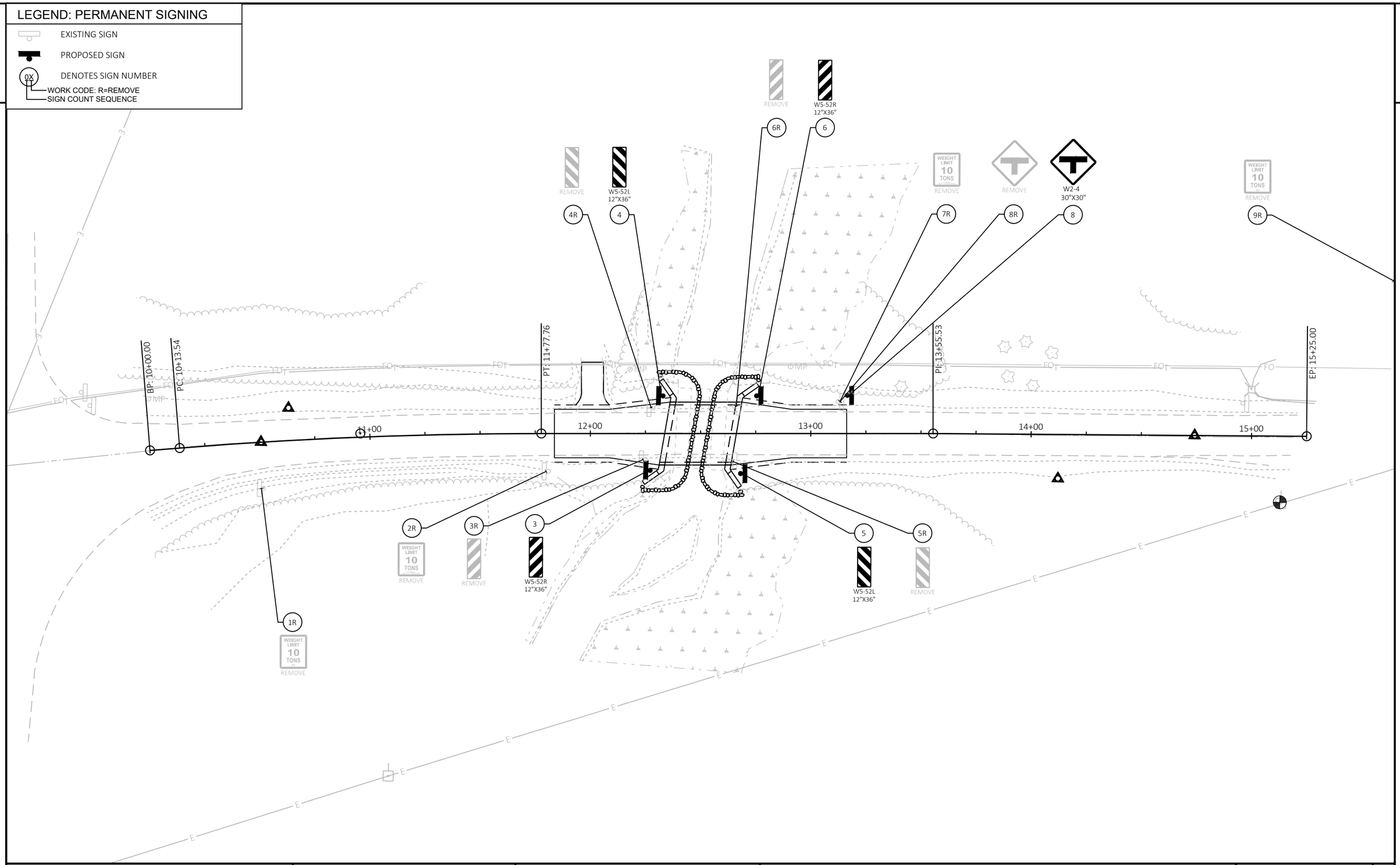


PROJECT NO: 8724-04-71	HWY: LOC STR	COUNTY: PRICE	REMOVAL DETAIL: STAGE 2	SHEET 5	E

FILE NAME : G:\00-PROJECT FILES\2024\24195 ID 8724-04-01, T OGEMA, LARKIN RD, PRICE CO\0-CAD\SHEETS\021101_RM.DWG
LAYOUT NAME - 021102_rm
PLOT DATE : 10/9/2025 3:56 PM PLOT BY : KYLE MAYBERRY PLOT NAME : PLOT SCALE : 1 IN:20 FT
WISDOT/CADDS SHEET 42

LEGEND: PERMANENT SIGNING

- EXISTING SIGN
- PROPOSED SIGN
- 0X — DENOTES SIGN NUMBER
  - WORK CODE: R=REMOVE
  - SIGN COUNT SEQUENCE

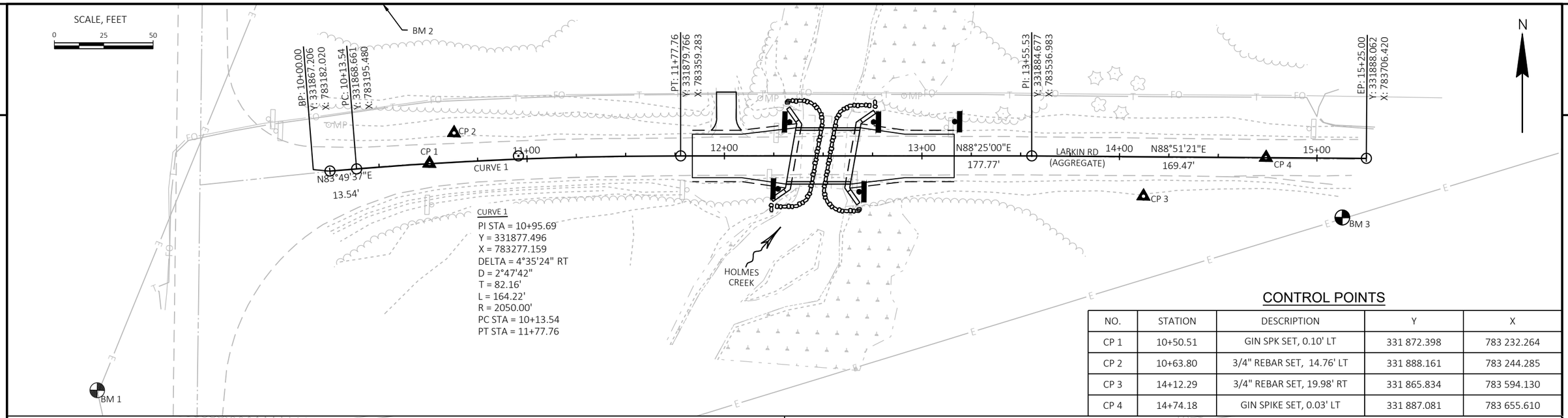


PROJECT NO: 8724-04-71	HWY: LARKIN RD	COUNTY: PRICE	PERMANENT SIGNING	SHEET 6	E

FILE NAME : G:\00-PROJECT FILES\2024\24195 ID 8724-04-01, T OGEMA, LARKIN RD, PRICE CO\0-CAD\SHEETS\023201_PS.DWG
LAYOUT NAME - 023201_ps

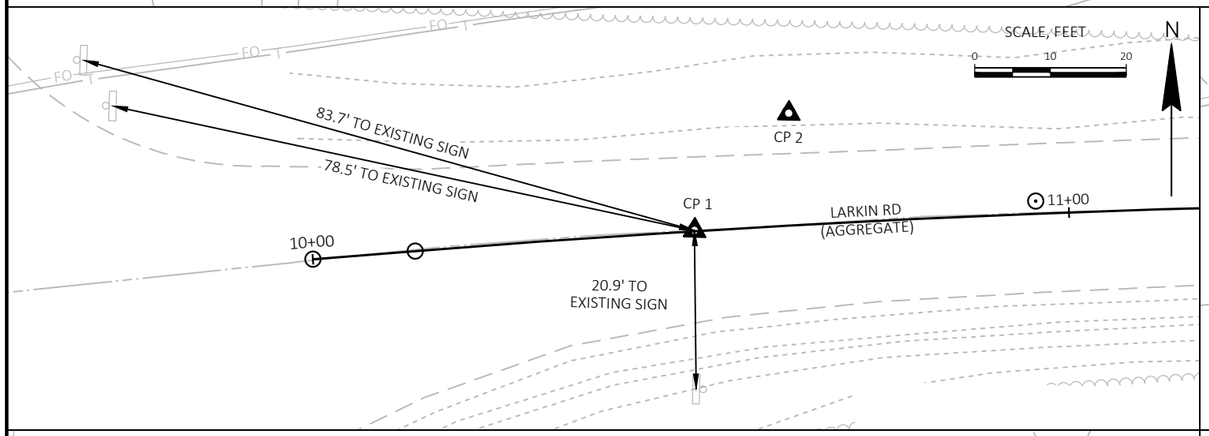
PLOT DATE : 10/9/2025 4:07 PM PLOT BY : KYLE MAYBERRY PLOT NAME : PLOT SCALE : 1 IN:40 FT

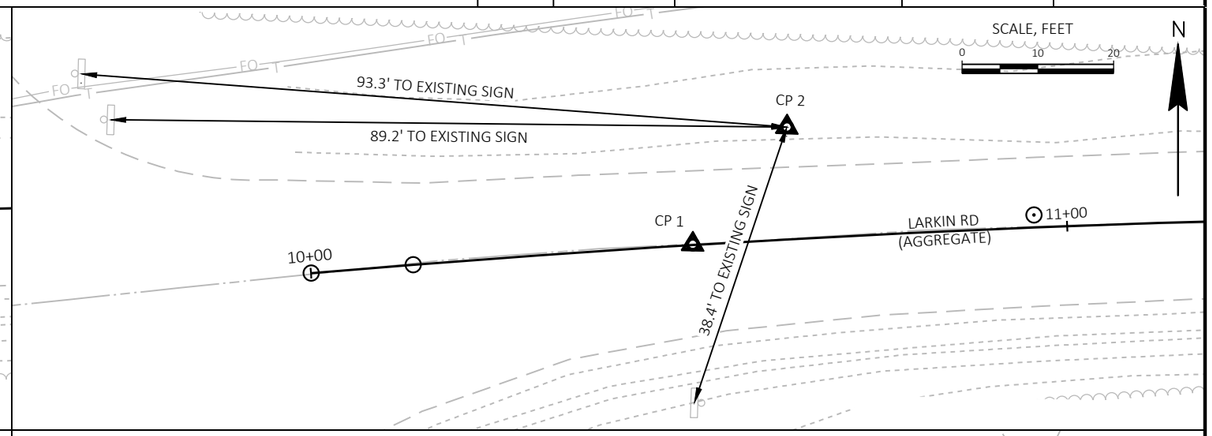
WISDOT/CADDS SHEET 42

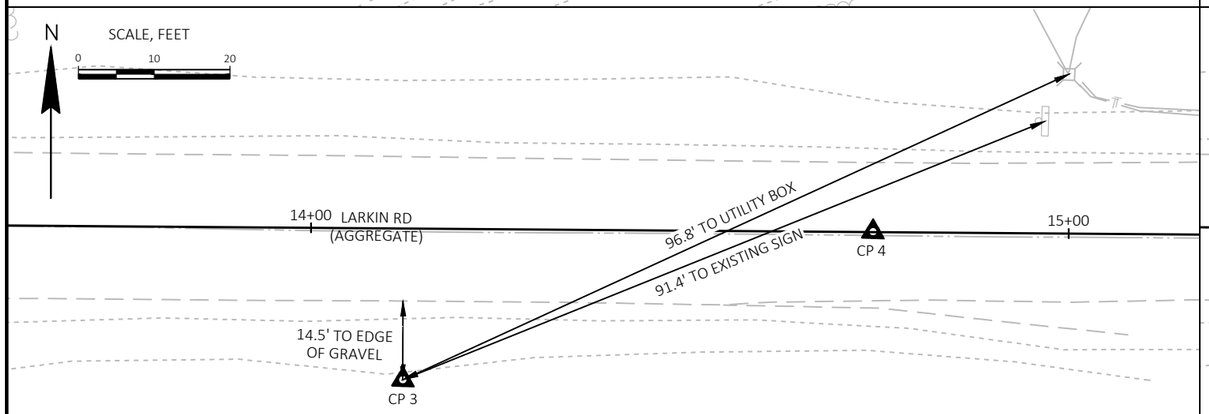


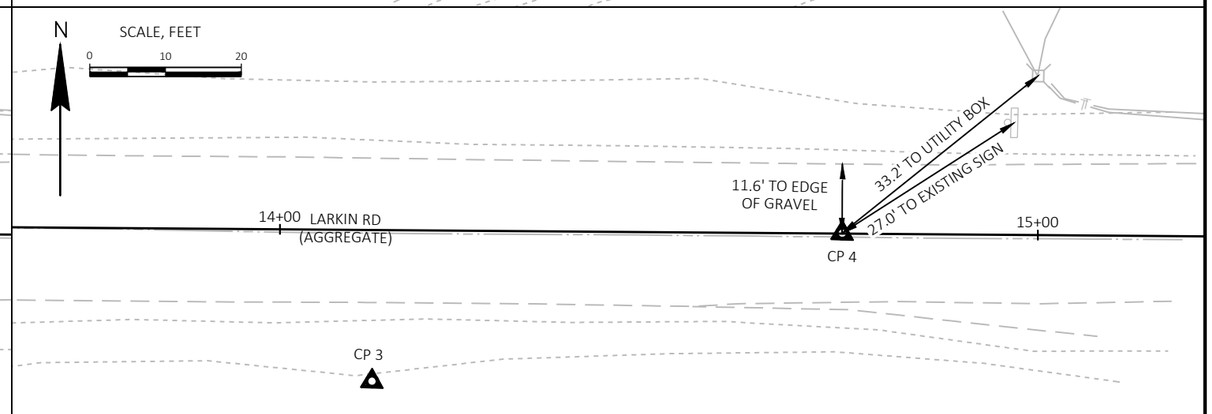
CONTROL POINTS

NO.	STATION	DESCRIPTION	Y	X
CP 1	10+50.51	GIN SPK SET, 0.10' LT	331 872.398	783 232.264
CP 2	10+63.80	3/4" REBAR SET, 14.76' LT	331 888.161	783 244.285
CP 3	14+12.29	3/4" REBAR SET, 19.98' RT	331 865.834	783 594.130
CP 4	14+74.18	GIN SPIKE SET, 0.03' LT	331 887.081	783 655.610









PROJECT NO: 8724-04-71	HWY: LOC STR	COUNTY: PRICE	ALIGNMENT DETAILS	SHEET 7	E

Estimate Of Quantities

8724-04-71

Line	Item	Item Description	Unit	Total	Qty
0002	201.0205	Grubbing	STA	3.000	3.000
0004	203.0220	Removing Structure (structure) 01. P-50-0914	EACH	1.000	1.000
0006	205.0100	Excavation Common	CY	85.000	85.000
0008	206.1001	Excavation for Structures Bridges (structure) 01. B-50-0097	EACH	1.000	1.000
0010	208.0100	Borrow	CY	44.000	44.000
0012	210.1500	Backfill Structure Type A	TON	300.000	300.000
0014	213.0100	Finishing Roadway (project) 01. 8724-04-71	EACH	1.000	1.000
0016	305.0110	Base Aggregate Dense 3/4-Inch	TON	14.000	14.000
0018	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	170.000	170.000
0020	455.0605	Tack Coat	GAL	20.000	20.000
0022	465.0105	Asphaltic Surface	TON	60.000	60.000
0024	502.0100	Concrete Masonry Bridges	CY	108.000	108.000
0026	502.3200	Protective Surface Treatment	SY	149.000	149.000
0028	505.0400	Bar Steel Reinforcement HS Structures	LB	4,250.000	4,250.000
0030	505.0600	Bar Steel Reinforcement HS Coated Structures	LB	13,020.000	13,020.000
0032	513.4061	Railing Tubular Type M	LF	70.000	70.000
0034	516.0500	Rubberized Membrane Waterproofing	SY	12.000	12.000
0036	550.0500	Pile Points	EACH	14.000	14.000
0038	550.2106	Piling CIP Concrete 10 3/4 X 0.365-Inch	LF	560.000	560.000
0040	606.0300	Riprap Heavy	CY	130.000	130.000
0042	612.0406	Pipe Underdrain Wrapped 6-Inch	LF	160.000	160.000
0044	618.0100	Maintenance and Repair of Haul Roads (project) 01. 8724-04-71	EACH	1.000	1.000
0046	619.1000	Mobilization	EACH	1.000	1.000
0048	624.0100	Water	MGAL	2.000	2.000
0050	625.0500	Salvaged Topsoil	SY	215.000	215.000
0052	628.1104	Erosion Bales	EACH	2.000	2.000
0054	628.1504	Silt Fence	LF	250.000	250.000
0056	628.1520	Silt Fence Maintenance	LF	396.000	396.000
0058	628.1905	Mobilizations Erosion Control	EACH	8.000	8.000
0060	628.1910	Mobilizations Emergency Erosion Control	EACH	4.000	4.000
0062	628.2008	Erosion Mat Urban Class I Type B	SY	215.000	215.000
0064	628.6005	Turbidity Barriers	SY	180.000	180.000
0066	629.0210	Fertilizer Type B	CWT	0.500	0.500
0068	630.0130	Seeding Mixture No. 30	LB	20.000	20.000
0070	630.0200	Seeding Temporary	LB	13.000	13.000
0072	630.0500	Seed Water	MGAL	10.000	10.000
0074	634.0612	Posts Wood 4x6-Inch X 12-FT	EACH	5.000	5.000
0076	637.2230	Signs Type II Reflective F	SF	18.250	18.250
0078	638.2602	Removing Signs Type II	EACH	9.000	9.000
0080	638.3000	Removing Small Sign Supports	EACH	8.000	8.000
0082	642.5001	Field Office Type B	EACH	1.000	1.000
0084	643.0420	Traffic Control Barricades Type III	DAY	944.000	944.000
0086	643.0705	Traffic Control Warning Lights Type A	DAY	1,888.000	1,888.000
0088	643.0900	Traffic Control Signs	DAY	885.000	885.000
0090	643.1050	Traffic Control Signs PCMS	DAY	14.000	14.000
0092	643.5000	Traffic Control	EACH	1.000	1.000
0094	645.0111	Geotextile Type DF Schedule A	SY	64.000	64.000
0096	645.0120	Geotextile Type HR	SY	210.000	210.000
0098	650.4500	Construction Staking Subgrade	LF	100.000	100.000

Estimate Of Quantities

8724-04-71

Line	Item	Item Description	Unit	Total	Qty
0100	650.5000	Construction Staking Base	LF	100.000	100.000
0102	650.6501	Construction Staking Structure Layout (structure) 01. B-50-0097	EACH	1.000	1.000
0104	650.9911	Construction Staking Supplemental Control (project) 01. 8724-04-71	EACH	1.000	1.000
0106	650.9920	Construction Staking Slope Stakes	LF	100.000	100.000
0108	715.0502	Incentive Strength Concrete Structures	DOL	648.000	648.000
0110	SPV.0090	Special 01. Flashing Stainless Steel	LF	55.000	55.000
0112	SPV.0195	Special 01. Infill Riprap B-50-097	TON	100.000	100.000

EXCAVATION SUMMARY

DIVISION	FROM/TO STATION	205.0100 EXCAVATION COMMON (1) CUT (2)	SALVAGED/UNUSABLE PAVEMENT MATERIAL (3)	AVAILABLE MATERIAL (4)	UNEXPANDED FILL	EXPANDED FILL (5) FACTOR 1.25	MASS ORDINATE +/- (6)	WASTE (7)	208.0100 BORROW
WEST APPROACH	11+83.73/12+33.73	42	0	42	48	60	-18	0	18
EAST APPROACH	12+66.27/13+16.27	43	0	43	55	69	-26	0	26
GRAND TOTAL		85	0	85	103	129	-44	0	44

NOTES:
(1) EXCAVATION COMMON IS THE SUM OF THE CUT COLUMN. ITEM NUMBER 205.0100
(2) SALVAGED/UNSUABLE PAVEMENT MATERIAL IS INCLUDED IN CUT.
(3) SALVAGED/UNUSABLE PAVEMENT MATERIAL INCLUDES EXISTING ASPHALT.
(4) AVAILABLE MATERIAL = CUT - SALVAGED/UNUSABLE PAVEMENT MATERIAL
(5) EXPANDED FILL FACTOR = 1.25
EXPANDED FILL = (UNEXPANDED FILL) * FILL FACTOR
(6) THE MASS ORDINATE + OR - QTY CALCULATED FOR THE DIVISION. PLUS QUANTITY INDICATES AN EXCESS OF MATERIAL WITHIN THE DIVISION. MINUS INDICATES A SHORTAGE OF MATERIAL WITHIN THE DIVISION.
(7) FACTORS USED TO COMPUTE ANTICIPATED WASTE AND THE COMPUTED WASTE VOLUME IDENTIFIED ARE FOR GENERAL INFORMATION ONLY.

BASE AGGREGATE DENSE

STATION	TO	STATION	LOCATION	305.0110 BASE AGGREGATE DENSE 3/4-INCH TON	305.0120 BASE AGGREGATE DENSE 1 1/4-INCH TON	624.0100 WATER MGAL
11+84	-	12+34	WEST APPROACH	7	85	1.0
12+66	-	13+16	EAST APPROACH	7	85	1.0
			TOTAL	14	170	2.0

ASPHALTIC SURFACE

STATION	TO	STATION	LOCATION	455.0605 TACK COAT GAL	465.0105 ASPHALTIC SURFACE TON
11+84	-	12+34	WEST APPROACH	10	30
12+66	-	13+16	EAST APPROACH	10	30
			TOTAL	20	60

GRUBBING

STATION	TO	STATION	LOCATION	201.0205 STA
11+84	-	13+16	PROJECT	3
			TOTAL	3

FINISHING ITEMS

STATION	TO	STATION	LOCATION	625.0500 SALVAGED TOPSOIL SY	628.2008 EROSION MAT URBAN CLASS I TYPE B SY	629.0210 FERTILIZER TYPE B CWT	630.0130 SEEDING MIXTURE NO. 30 LB	630.0200 SEEDING TEMPORARY LB	630.0500 SEED WATER MGAL
11+84	-	12+34	WEST APPROACH, LT	34	34	0.1	4	2	1.70
11+84	-	12+26	WEST APPROACH, RT	49	49	0.1	5	3	2.20
12+74	-	13+16	EAST APPROACH, LT	40	40	0.1	4	3	1.90
12+66	-	13+16	EAST APPROACH, RT	49	49	0.1	5	3	2.30
			UNDISTRIBUTED	43	43	0.1	2	2	1.90
			TOTAL	215	215	0.5	20	13	10

SILT FENCE

STATION	TO	STATION	LOCATION	628.1504 SILT FENCE LF	628.1520 SILT FENCE MAINTENANCE LF
11+82	-	12+39	WEST APPROACH, LT	52	104
12+01	-	12+25	WEST APPROACH, RT	25	50
12+60	-	13+21	EAST APPROACH, LT	58	116
12+65	-	13+21	EAST APPROACH, RT	63	126
			UNDISTRIBUTED	52	---
			TOTAL	250	396

MOBILIZATIONS EROSION CONTROL

LOCATION	628.1905 MOBILIZATIONS EROSION CONTROL EACH	628.1910 MOBILIZATIONS EMERGENCY EROSION CONTROL EACH
PROJECT	8	4
TOTAL	8	4

EROSION BALES

STATION	LOCATION	628.1104 EACH
12+00	WEST APPROACH, LT	1
	UNDISTRIBUTED	1
	TOTAL	2

TURBIDITY BARRIERS

LOCATION	628.6005 SY
NORTH WEST APPROACH	27
SOUTH WEST APPROACH	48
EAST APPROACH	76
UNDISTRIBUTED	29
TOTAL	180

ALL ITEMS CATEGORY 0010 UNLESS NOTED OTHERWISE.

PERMANENT SIGNING

STATION	LOCATION	SIGN NUMBER	SIGN CODE	SIGN DESCRIPTION	634.0612 POSTS WOOD 4X6-INCH X 12-FT EACH	637.2230 SIGNS TYPE II REFLECTIVE F SF	638.2602 REMOVING SIGNS TYPE II EACH	638.3000 REMOVING SMALL SIGN SUPPORTS EACH	REMARKS
10+50	RT	1R	R12-1	WEIGHT LIMIT 10 TON 100 FEET AHEAD	---	---	1	1	
11+80	RT	2R	R12-1	WEIGHT LIMIT 10 TON	---	---	1	1	
12+24	RT	3	W5-52R	BRIDGE HASH MARK	1	3	---	---	
12+26	RT	3R	W5-52R	BRIDGE HASH MARK	---	---	1	1	
12+28	LT	4	W5-52L	BRIDGE HASH MARK	1	3	---	---	
12+32	LT	4R	W5-52L	BRIDGE HASH MARK	---	---	1	1	
12+69	RT	5	W5-52R	BRIDGE HASH MARK	1	3	---	---	
12+65	RT	5R	W5-52R	BRIDGE HASH MARK	---	---	1	1	
12+76	LT	6	W5-52L	BRIDGE HASH MARK	1	3	---	---	
12+66	LT	6R	W5-52L	BRIDGE HASH MARK	---	---	1	1	
13+13	LT	7R	R-12-1	WEIGHT LIMIT 10 TONS	---	---	1	1	
13+13	LT	8R	W2-4	"T" INTERSECTION AHEAD	---	---	1	---	SAME POST AS "WEIGHT LIMIT 10 TONS" SIGN
13+17	LT	8	W2-4	"T" INTERSECTION AHEAD	1	6.25	---	---	
---	---	9R	R12-1	WEIGHT LIMIT 10 TON AHEAD	---	---	1	1	
			TOTAL		5	18.25	9	8	

3

3

TRAFFIC CONTROL

LOCATION	DURATION DAY	643.0420 TRAFFIC CONTROL BARRICADES TYPE III NO.	643.0420 TRAFFIC CONTROL BARRICADES TYPE III DAY	643.0705 TRAFFIC CONTROL WARNING LIGHTS TYPE A NO.	643.0705 TRAFFIC CONTROL WARNING LIGHTS TYPE A DAY	643.0900 TRAFFIC CONTROL SIGNS NO.	643.0900 TRAFFIC CONTROL SIGNS DAY	643.1050 TRAFFIC CONTROL SIGNS PCMS NO.	643.1050 TRAFFIC CONTROL SIGNS PCMS DAY	643.5000 TRAFFIC CONTROL EACH
WEST APPROACH	59	7	413	14	826	4	236	---	---	---
EAST APPROACH	59	7	413	14	826	9	531	---	---	---
UNDISTRIBUTED	59	2	118	4	236	2	118	---	---	---
PROJECT	---	---	---	---	---	---	---	2	14	1
TOTAL		16	944	32	1,888	15	885	2	14	1

CONSTRUCTION STAKING

STATION	TO	STATION	LOCATION	650.4500 CONSTRUCTION STAKING SUBGRADE LF	650.5000 CONSTRUCTION STAKING BASE LF	650.6501.01 CONSTRUCTION STAKING STRUCTURE LAYOUT 01. B-50-0097 EACH	650.9911.01 CONSTRUCTION STAKING SUPPLEMENTAL CONTROL 01. 8724-04-71 EACH	650.9920 CONSTRUCTION STAKING SLOPE STAKES LF
11+84	-	12+34	WEST APPROACH	50	50	---	---	50
12+66	-	13+16	EAST APPROACH	50	50	---	---	50
			PROJECT	---	---	1	1	---
			TOTAL	100	100	1*	1	100

* CATEGORY 0020

INFILL RIPRAP B-50-097

LOCATION	SPV.0195.01 TON
WEST ABUTMENT	50
EAST ABUTMENT	50
TOTAL	100

ALL ITEMS CATEGORY 0010 UNLESS NOTED OTHERWISE.

LEGEND
- SLOPE INTERCEPT
- SURFACE WATER FLOW
- STREAM EDGE
- SILT FENCE
- EXISTING SIGN
- PROPOSED SIGN
- WETLANDS
- RIPRAP

KEVIN G. AND LAURA L. BLOMBERG

KEVIN G. AND LAURA L. BLOMBERG

N

TO BM 2

BP: 10+00.00

PC: 10+13.54

EXISTING R/W

STA 12+01
20' P.E.
(B.A.D.)

PT: 11+77.76

STREAM EDGE, TYP.

SLOPE INTERCEPT, TYP.

PT: 13+55.53

BRIGHTSPEED

EP: 15+25.00

CP 2

CP 1

11+00

12+00

13+00

14+00

15+00

LARKIN RD
(AGGREGATE)

CP 4

CP 3

BM 3

SECTION LINE

T 35 N R 1 E
T 34 N R 1 E
33 34
4 3

TO BM 1

HALLSTRAND RD
(AGGREGATE)

SECTION LINE

PROPOSED STRUCTURE
B-50-0097

HOLMES
CREEK

EXISTING STRUCTURE
P-50-0914

SILT FENCE, TYP.

EXISTING R/W

PRICE ELECTRIC
COOPERATIVE

BEGIN PROJECT
STA 11+83.73
MATCH EXISTING
Y: 331 880.009
X: 783 365.248

LEROY GUSTAFSON

WILLIAM E. GUSTAFSON

END PROJECT
STA 13+16.27
MATCH EXISTING

BENCH MARKS			
NO.	STATION	ELEV.	DESCRIPTION
BM 1	N/A	1483.62	RAILROAD SPIKE IN POWER POLE
BM 2	10+11.04	1458.62	RAILROAD SPIKE IN POWER POLE, 169' LT
BM 3	15+13.01	1464.36	BARN SPIKE IN POWER POLE, 30' RT

9+50.00 10+00.00 11+00.00 12+00.00 13+00.00 14+00.00 15+00.00 15+50.00

1465 1460 1455 1453

STA 11+83.73
EL 1465.76

VCL = 125.00

K = 50.01

-3.25%

PROPOSED ℄ PROFILE

STA 13+16.27
EL 1463.15

-0.75%

EXISTING ℄ PROFILE

VPC STA 11+85.76
VPC EL 1465.69

VPI STA 12+48.26
VPI EL 1463.66

HIGH WATER$_{100}$
EL. = 1461.76

VPT STA 13+10.76
VPT EL 1463.19

OBSERVED WATER
EL. = 1455.68
(11-14-2024)

EXISTING ℄ ELEVATION

PROPOSED ℄ ELEVATION

1472.20 1470.47 1468.71 1466.95 1465.25 **1465.25** 1463.91 **1464.02** 1463.30 **1463.29** 1462.98 1463.31 1463.70 1464.36

STA. 12+50.00
REMOVE EXISTING STRUCTURE P-50-0914
22.2-FT OVERALL LENGTH AND
22.0-FT CLEAR ROADWAY WIDTH
THREE 6.0-FT STEEL CULVERT PIPES

STA. 12+50.00
STRUCTURE B-50-0097
32.54-FT OVERALL LENGTH
26.00-FT CLEAR ROADWAY WIDTH
SINGLE SPAN CONCRETE FLAT SLAB STRUCTURE

PROJECT NO: 8724-04-71	HWY: LOC STR	COUNTY: PRICE	PLAN AND PROFILE	SHEET 12	E

FILE NAME : G:\00-PROJECT FILES\2024\24195 ID 8724-04-01, T OGEMA, LARKIN RD, PRICE CO\0-CAD\SHEETS\050101_PP.DWG
LAYOUT NAME - 050101_pp

PLOT DATE : 10/10/2025 7:44 AM PLOT BY : KYLE MAYBERRY PLOT NAME : PLOT SCALE : 1 IN:40 FT

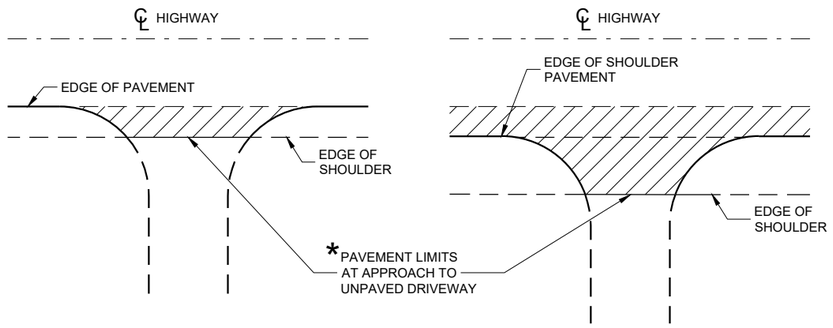
WISDOT/CADDS SHEET 44

Standard Detail Drawing List

08D21-01	DRIVEWAYS WITHOUT CURB & GUTTER
08E08-03	TYPICAL INSTALLATIONS OF EROSION BALES / TEMPORARY DITCH CHECKS
08E09-06	SILT FENCE
08E11-02	TURBIDITY BARRIER
12A03-10	NAME PLATE (STRUCTURES)
13C19-03	HMA LONGITUDINAL JOINTS
15C02-09A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-09B	BARRICADES AND SIGNS FOR VARIOUS CLOSURES
15C06-12	SIGNING & MARKING FOR TWO LANE BRIDGES
15C11-10B	CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS

6

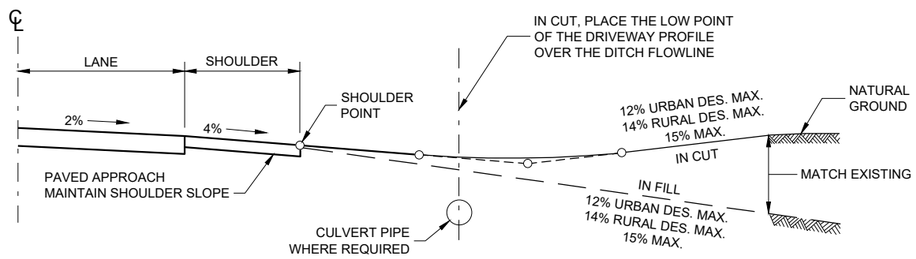
6



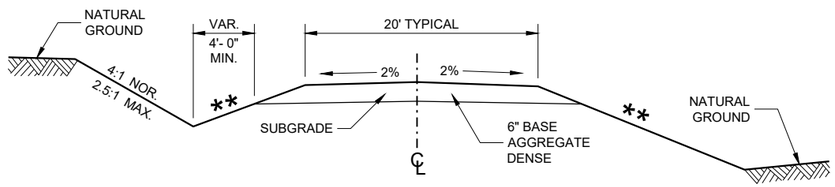
PLAN VIEW
(UNPAVED SHOULDER ON HIGHWAY)

PLAN VIEW
(PAVED SHOULDER ON HIGHWAY)

RURAL DRIVEWAY INTERSECTION DETAIL
(NO CURB AND GUTTER OR SIDEWALK)



TYPICAL DRIVEWAY PROFILES

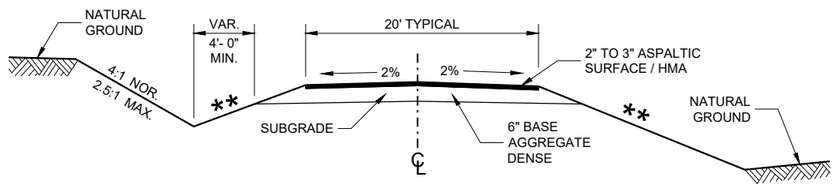


IN CUT **IN FILL**

TYPICAL CROSS SECTION FOR PRIVATE DRIVE OR FIELD ENTRANCE AGGREGATE SURFACE

** SLOPE CAN VARY WITH SPEED. SEE 11-45-30.6.2

POSTED SPEED MPH	MAX. SLOPE
<35	4:1
≥ 35 TO < 60	6:1
≥60	10:1



IN CUT **IN FILL**

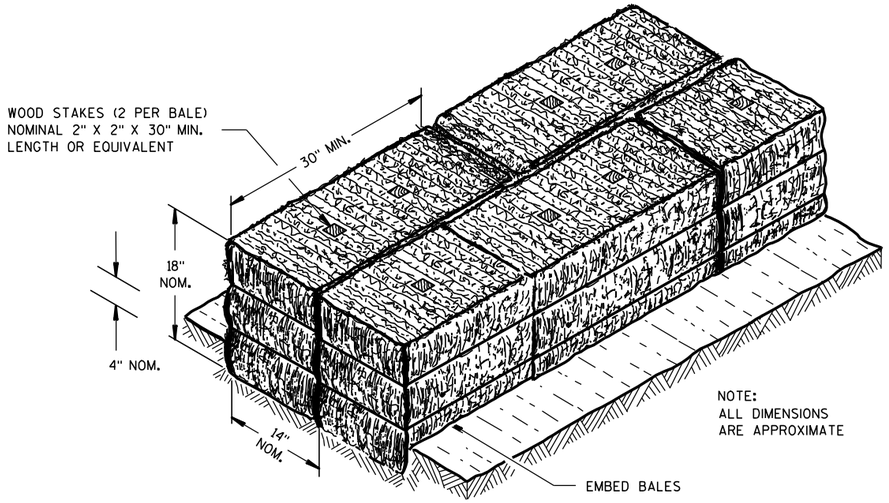
TYPICAL CROSS SECTION FOR PRIVATE DRIVE OR FIELD ENTRANCE ASPHALTIC SURFACE

DRIVEWAYS WITHOUT CURB AND GUTTER

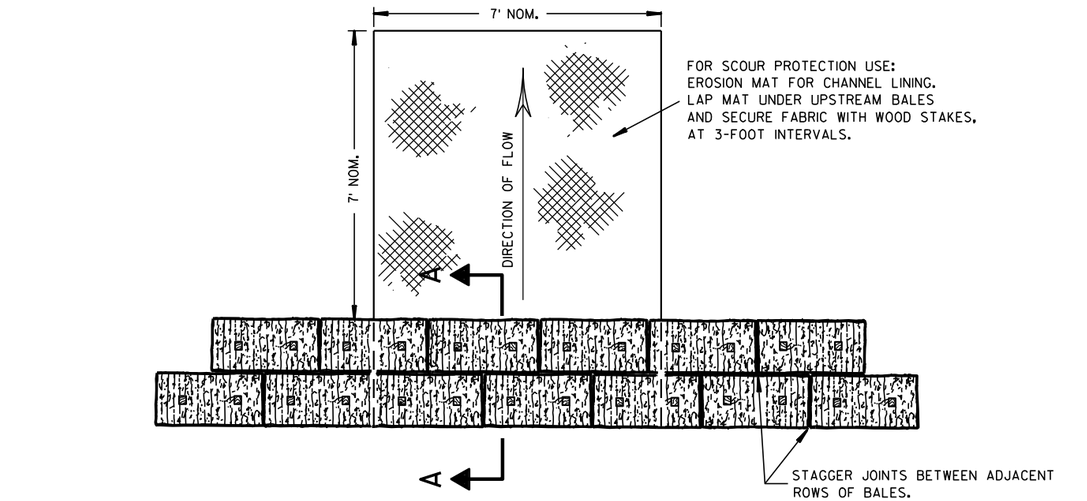
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
December 2017 — DATE
/S/ Rodney Taylor — ROADWAY STANDARDS DEVE UNIT SUPERVISOR
FHWA

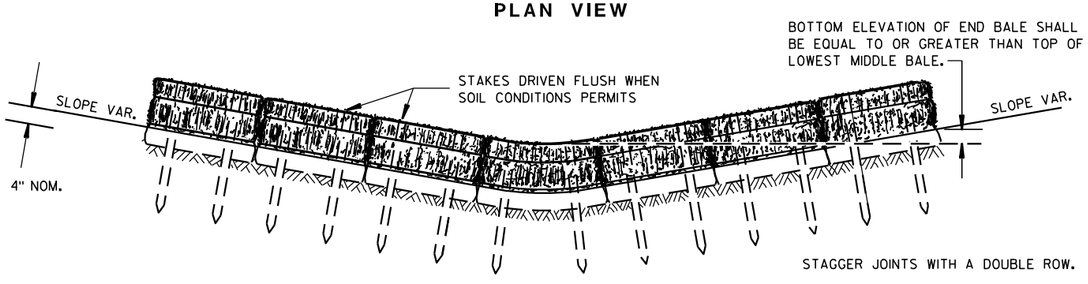
6
SDD 08D21 - 01



SECTION A-A



PLAN VIEW



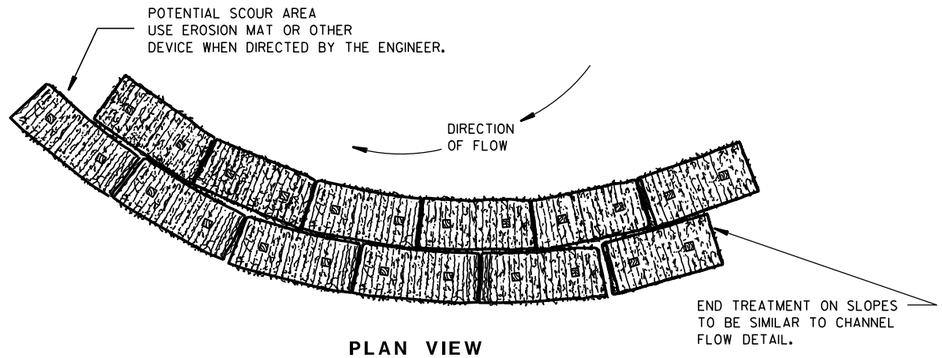
FRONT ELEVATION

TEMPORARY DITCH CHECK USING EROSION BALES (1)

GENERAL NOTES

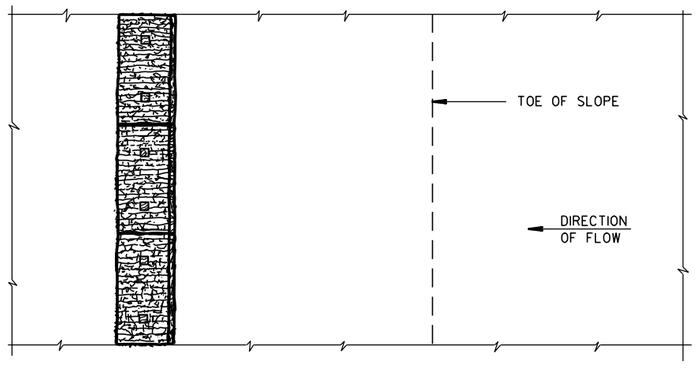
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

(1) TEMPORARY DITCH CHECKS EITHER EROSION BALES OR MANUFACTURED SHALL BE PAID FOR UNDER THE BID ITEM OF TEMPORARY DITCH CHECK. THE DEPARTMENT WILL NOT PAY FOR TEMPORARY DITCH CHECKS CONSTRUCTED OF A SINGLE ROW OF EROSION BALES.

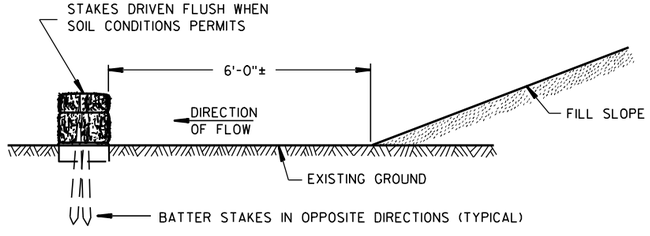


PLAN VIEW

WHEN ALTERING THE DIRECTION OF FLOW



PLAN VIEW



FRONT ELEVATION

WHEN EXISTING GROUND SLOPES AWAY FROM FILL SLOPE

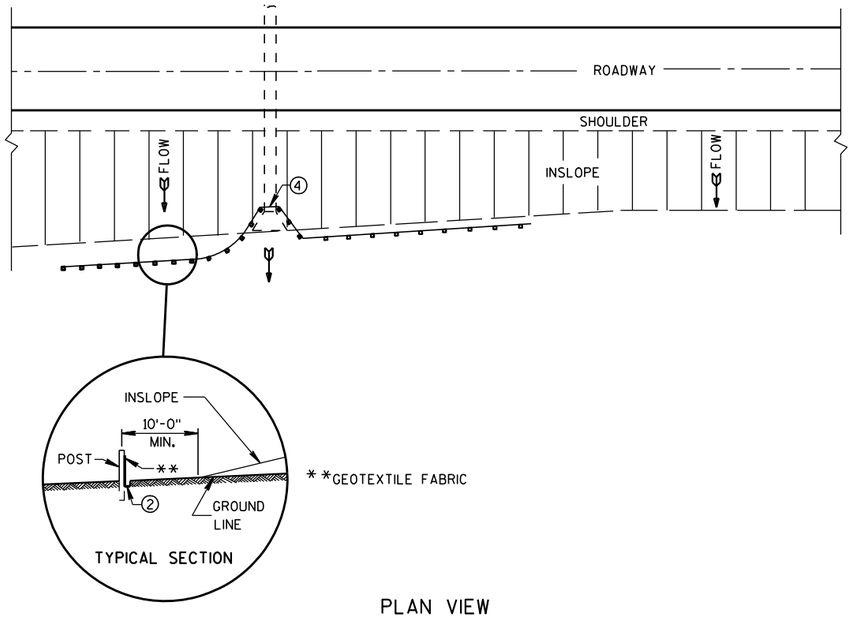
EROSION BALES FOR SHEET FLOW

TYPICAL INSTALLATIONS OF EROSION BALES / TEMPORARY DITCH CHECKS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

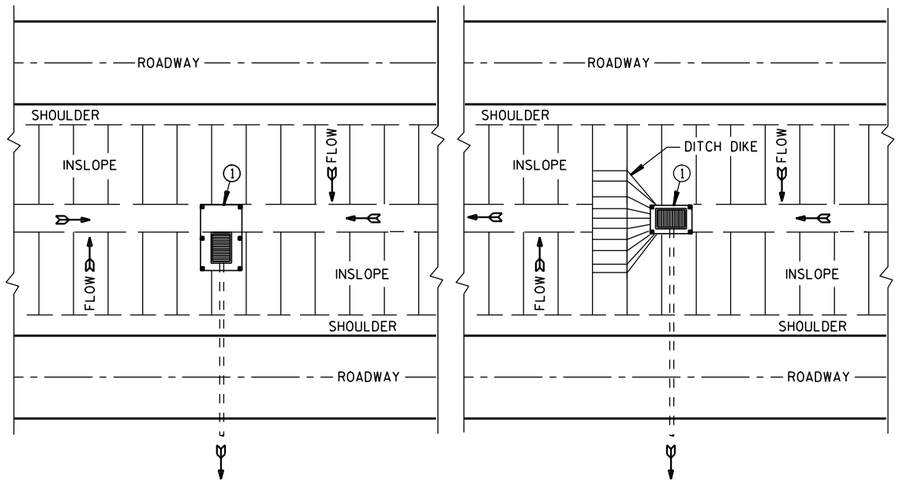
APPROVED
6/04/02 DATE
/S/ Beth Cannestra CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA

6
S.D.D. 8 E 8-3



PLAN VIEW

TYPICAL APPLICATION OF SILT FENCE



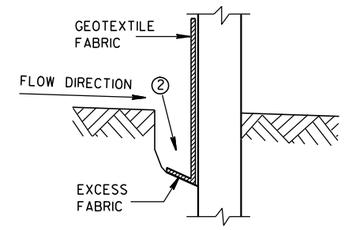
PLAN VIEW

SILT FENCE AT MEDIAN SURFACE DRAINS

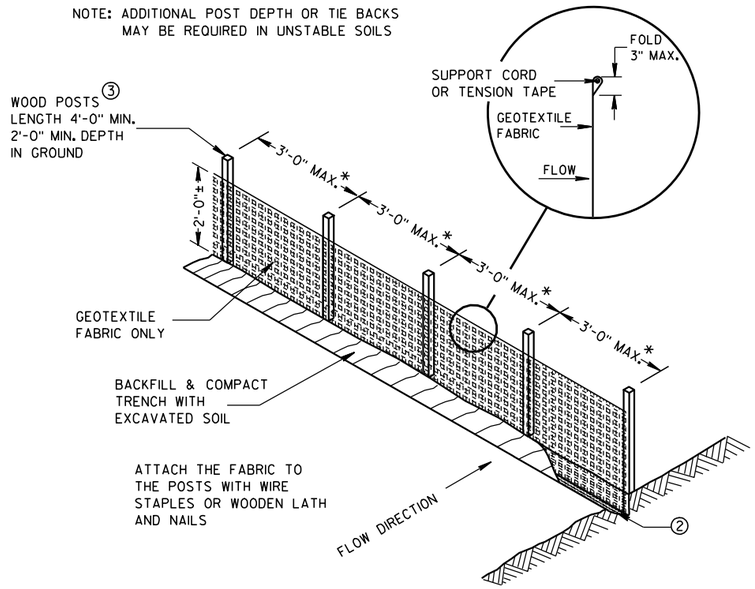
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

1. HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
2. FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
3. WOOD POSTS SHALL BE A MINIMUM SIZE OF 1⅛" X 1⅛" OF OAK OR HICKORY.
4. SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
5. CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.

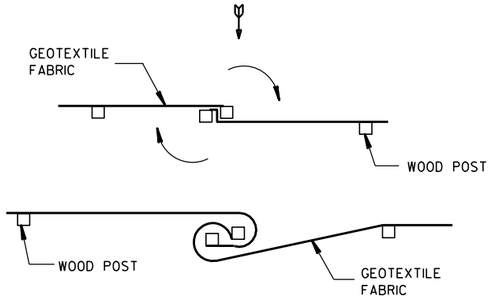


TRENCH DETAIL

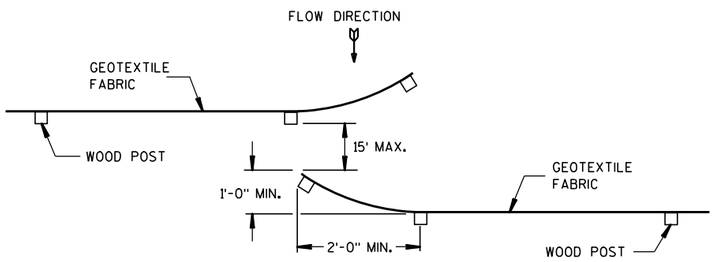


*NOTE: 8'-0" POST SPACING ALLOWED IF A WOVEN GEOTEXTILE FABRIC IS USED.

SILT FENCE

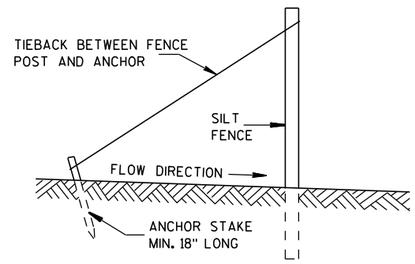


TWIST METHOD



HOOK METHOD

JOINING TWO LENGTHS OF SILT FENCE 5



SILT FENCE TIE BACK

(WHEN REQUIRED BY THE ENGINEER)

SILT FENCE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

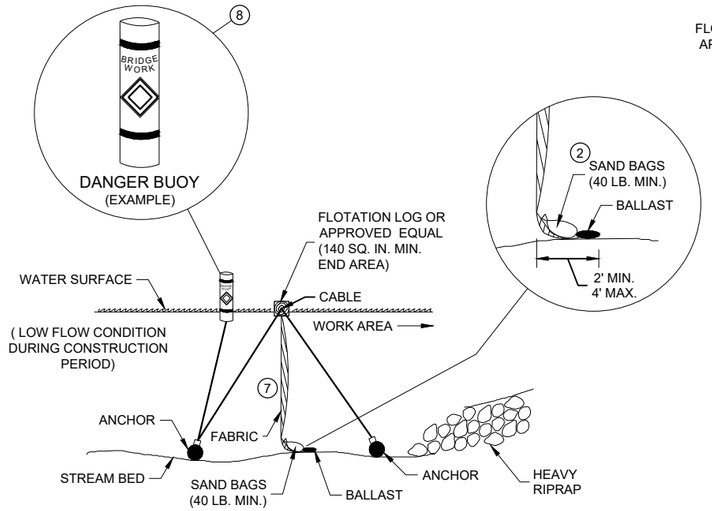
APPROVED
4-29-05 DATE
/S/ Beth Cannestra
CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA

6

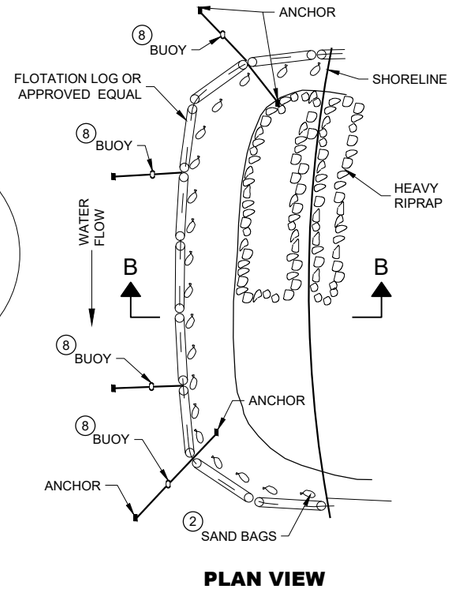
S.D.D. 8 E 9-6

6

S.D.D. 8 E 9-6

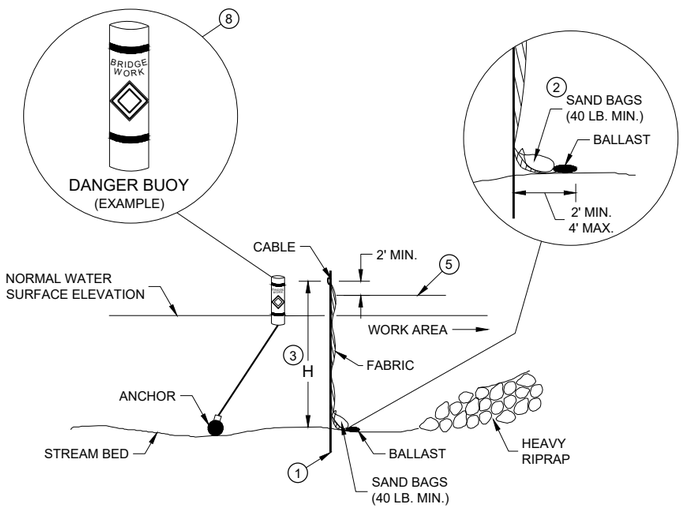


SECTION B - B

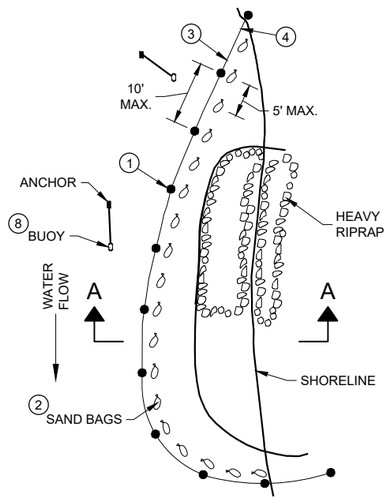


PLAN VIEW

TURBIDITY BARRIER - FLOAT ALTERNATIVE
CAUTION - SEE NOTE 6



SECTION A - A



PLAN VIEW

TURBIDITY BARRIER - STANDARD POST INSTALLATION

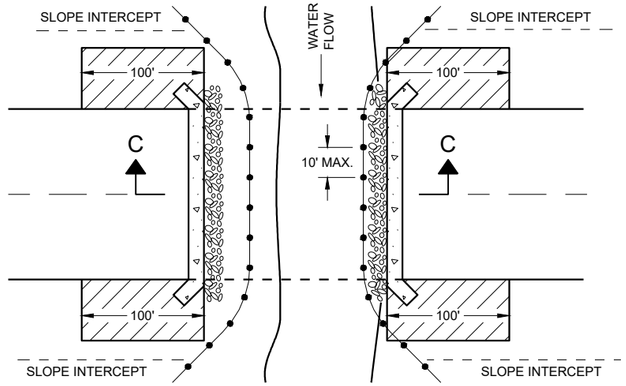
TURBIDITY BARRIER PLACEMENT DETAILS

GENERAL NOTES

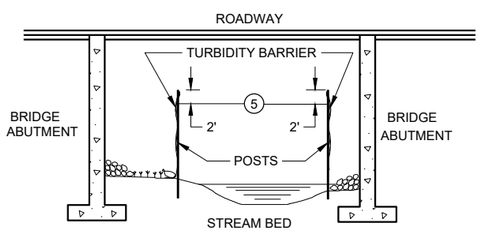
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

TURBIDITY BARRIER MAY BE REMOVED AT THE ENGINEERS DISCRETION, WHEN PERMANENT EROSION CONTROL MEASURES HAVE BEEN ESTABLISHED.

1. DRIVEN STEEL POSTS, PIPES, OR CHANNELS. LENGTH SHALL BE SUFFICIENT TO SECURELY SUPPORT BARRIER AT HIGH WATER ELEVATIONS.
2. SAND BAGS TO BE USED AS ADDITIONAL BALLAST WHEN ORDERED BY THE ENGINEER TO MEET ADVERSE FIELD CONDITIONS. SPACE AS APPROPRIATE FOR SITE CONDITIONS.
3. WHEN BARRIER HEIGHT "H" EXCEEDS 8 FEET, POST SPACING MAY NEED TO BE DECREASED.
4. IN WATERWAYS SUBJECT TO FLUCTUATING WATER ELEVATIONS, PROVISIONS SHOULD BE MADE TO ALLOW THE WATER TO EQUALIZE ON EACH SIDE OF THE BARRIER. THIS MAY BE ACCOMPLISHED BY LEAVING A PORTION OF THE BARRIER OPEN ON THE UPSTREAM END.
5. ESTIMATED HIGH WATER ELEVATION DURING CONSTRUCTION PERIOD. MINIMUM BARRIER HEIGHT SHALL BE 2' GREATER THAN EITHER THE Q2 ELEVATION OR THE ESTIMATED HIGH WATER ELEVATION DURING CONSTRUCTION, WHICHEVER IS GREATER.
6. FLOAT ALTERNATIVE WILL ONLY BE ALLOWED WITH WRITTEN APPROVAL OF THE ENGINEER, AND IS MEANT FOR LOCATIONS WHERE BEDROCK PREVENTS THE INSTALLATION OF POSTS.
7. ALLOW SUFFICIENT SLACK VERTICALLY AND HORIZONTALLY SO THAT SEDIMENT BUILD UP WILL NOT SEPARATE OR LOWER THE TURBIDITY BARRIER.
8. USE AS DIRECTED BY COAST GUARD OR DNR PERMIT WHEN WORKING IN NAVIGABLE WATERWAYS.



PLAN VIEW



SECTION C - C

TURBIDITY BARRIER DETAIL SHOWING TYPICAL PLACEMENT AT STRUCTURES

TURBIDITY BARRIER

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
6/4/02
DATE
/S/ Beth Cannestra
CHIEF ROADWAY DEVELOI
ENGINEER

FHWA

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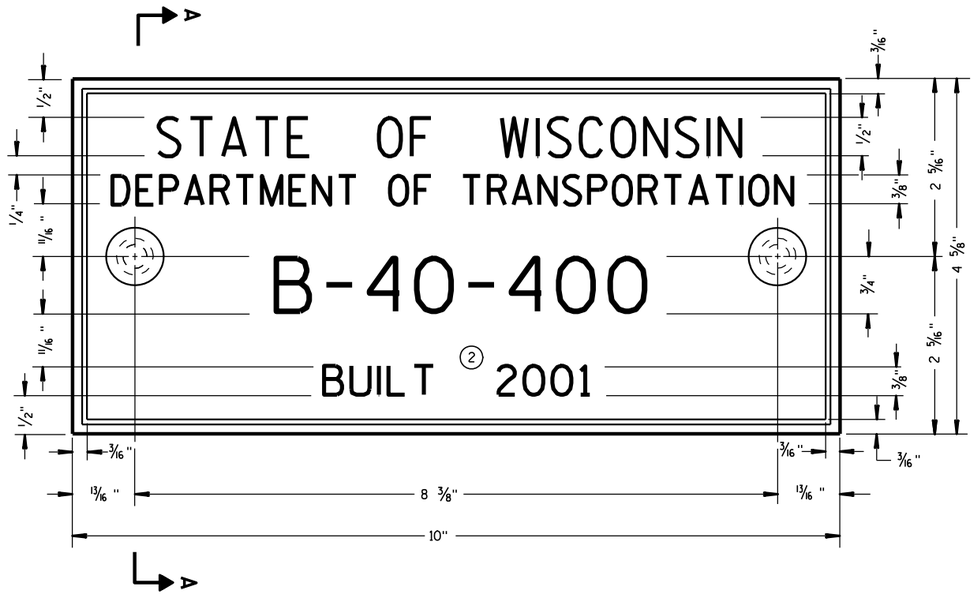
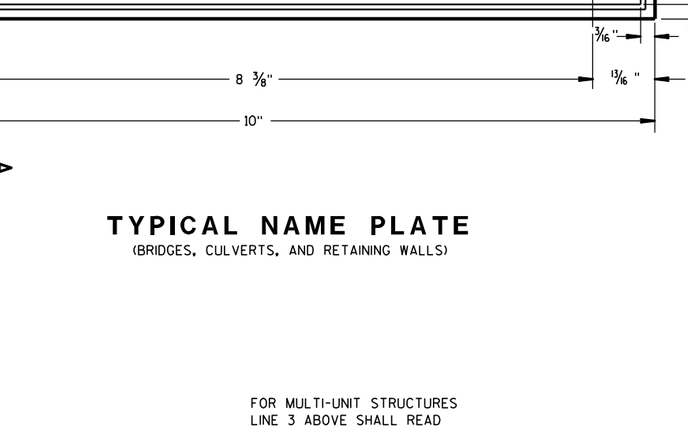
SDD 08E11 - 02



TYPICAL NAME PLATE

(BRIDGES, CULVERTS, AND RETAINING WALLS)



NUMBERING DESIGNATION MULTI-UNIT STRUCTURES

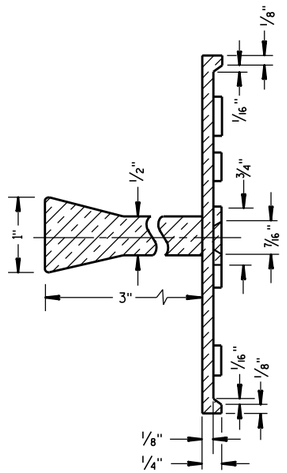
GENERAL NOTES

NAME PLATES TO BE INSTALLED ON BRIDGES, CULVERTS, AND RETAINING WALLS SHALL CONFORM TO THE REQUIREMENTS OF SECTION 502.3.11 OF THE STANDARD SPECIFICATIONS.

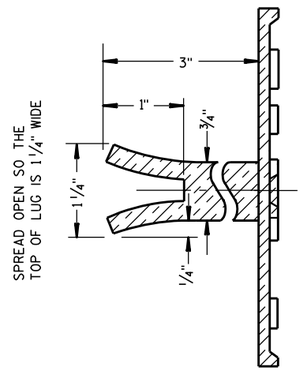
THE BRIDGE NUMBER AND YEAR BUILT SHOWN ON THIS DRAWING ARE EXAMPLES ONLY. SEE CONSTRUCTION PLANS FOR INDIVIDUAL NUMBERING AND YEAR BUILT.

(1) EPOXY RESIN SHALL BE FROM AN APPROVED MANUFACTURER AND USED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS.

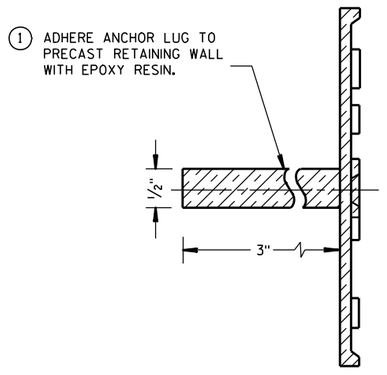
(2) REHABILITATION OF AN EXISTING STRUCTURE SHOULD USE THE DATE OF ORIGINAL STRUCTURE CONSTRUCTION.



SECTION A-A



ALTERNATE LUG



ALTERNATE LUG

(FOR ATTACHMENT TO PRECAST STRUCTURES)

NAME PLATE (STRUCTURES)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

3/26/10 DATE

/S/ Scot Beck -- CHIEF STRUCTURAL DEVELOP ENGINEER

FHWA

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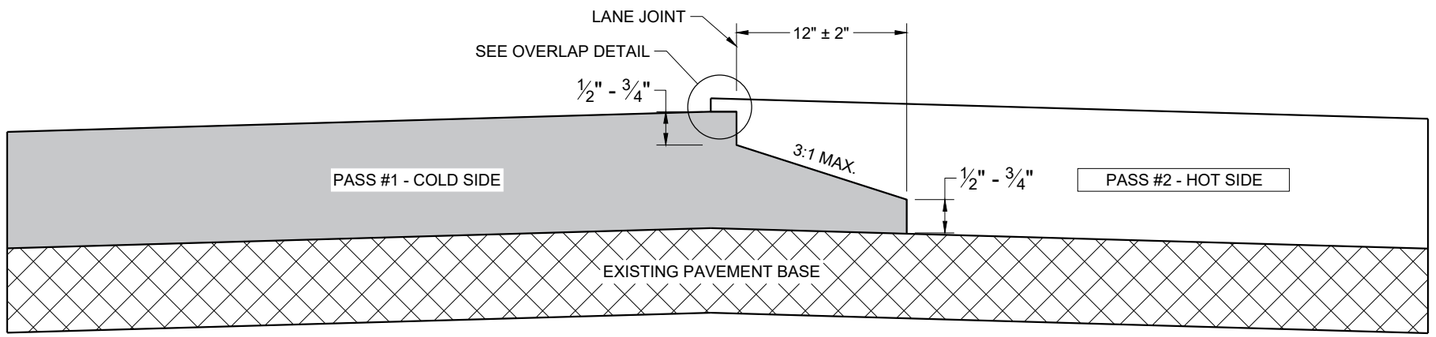
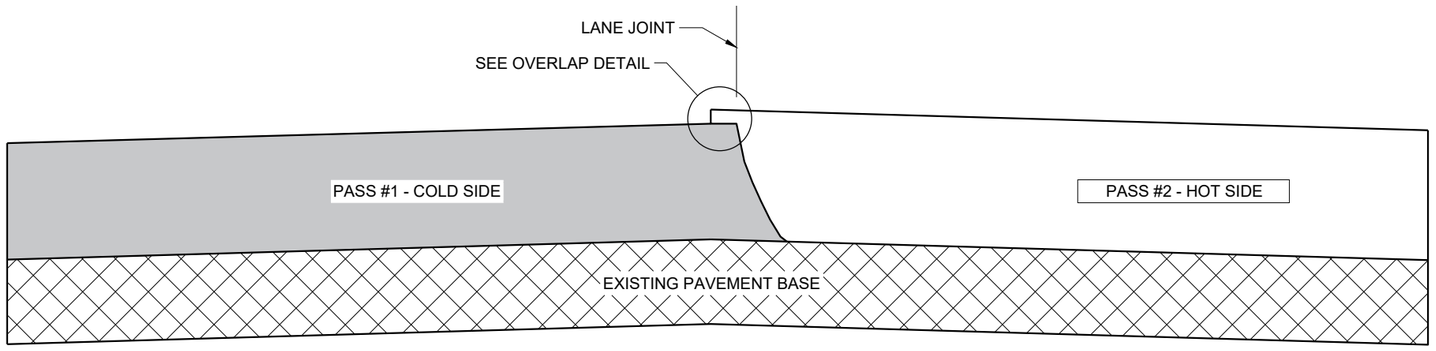
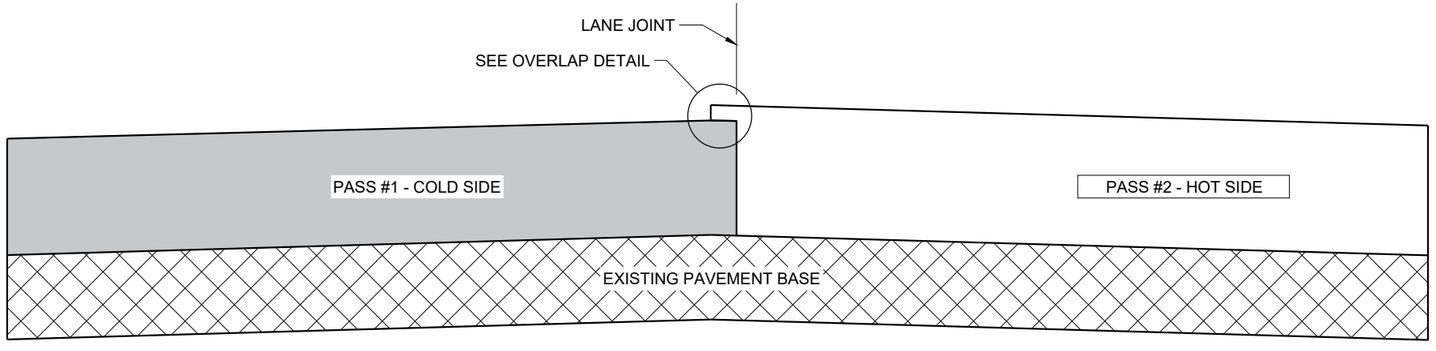
S.D.D. 12 A 3-10



TYPICAL PAVEMENT CROSS SECTION
NOTCHED WEDGE JOINT



TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT



TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT (MILLED)

GENERAL NOTES

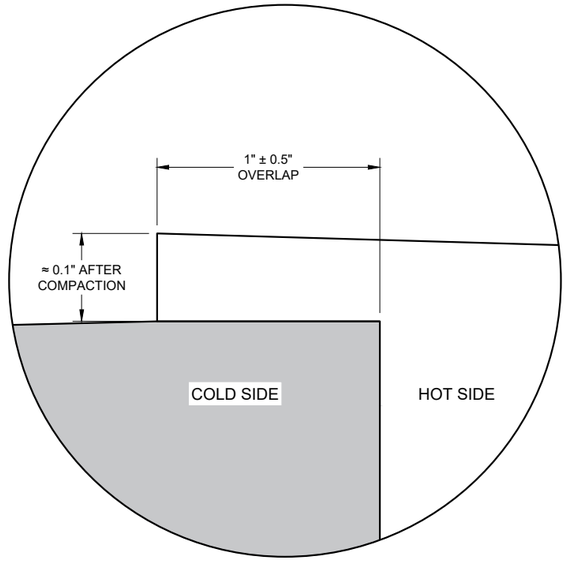
IN ADDITION TO THE DETAILS PROVIDED IN THIS DRAWING, CONFORM TO STANDARD SPECIFICATION 450.3.2.8 FOR WHEN A NOTCHED WEDGE JOINT IS REQUIRED AND FOR GENERAL JOINT CONSTRUCTION REQUIREMENTS.

FOR ALL LONGITUDINAL JOINTS, ENSURE THE PAVER SCREED OVERLAPS THE PREVIOUSLY PLACED PAVEMENT BY 1" ± 0.5" AND THE HOT SIDE OF THE JOINT REMAINS HIGHER THAN THE COLD SIDE BY APPROXIMATELY 0.1" AFTER FINAL COMPACTION. (IT WILL BE FLUSH WHEN PAVING IN ECHELON.)

ONLY REMOVE THE LONGITUDINAL NOTCHED WEDGE JOINT FOR SMA PAVEMENT OR AS DIRECTED BY THE ENGINEER TO ADDRESS SPECIFIC LENGTHS OF JOINT DAMAGED BY TRAFFIC.

WHEN MILLING BACK OR REMOVING ANY LONGITUDINAL JOINT, LIMIT THE MATERIAL REMOVED TO 2" FROM THE TOP NOTCH OR FROM THE VERTICAL JOINT EDGE ON THE COLD SIDE OF THE JOINT.

USE LONGITUDINAL MILLED JOINT AS PLANS SHOW OR THE AS THE ENGINEER DIRECTS.



OVERLAP DETAIL (TYPICAL)

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SDD 13C19 - 03

SDD 13C19 - 03

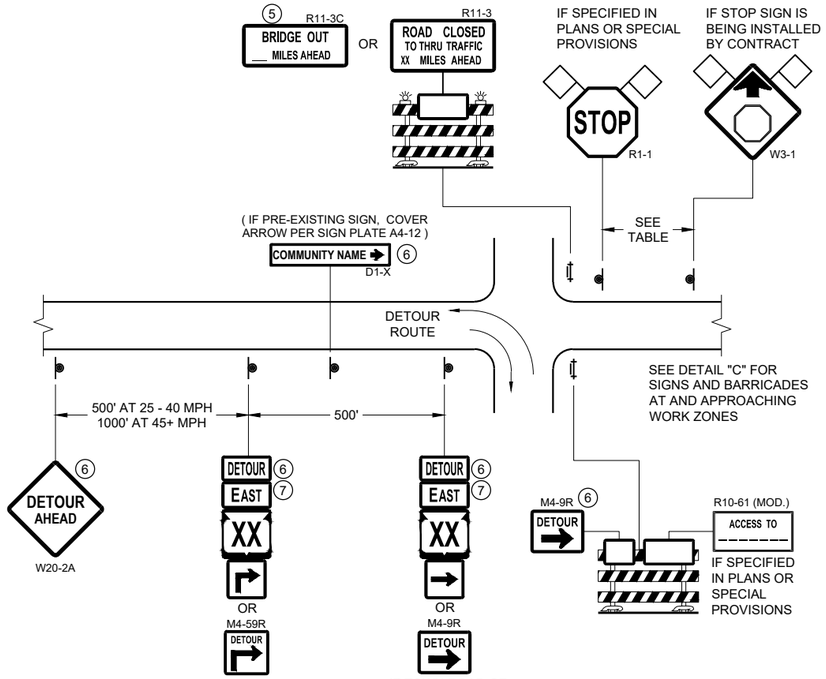
HMA LONGITUDINAL JOINTS

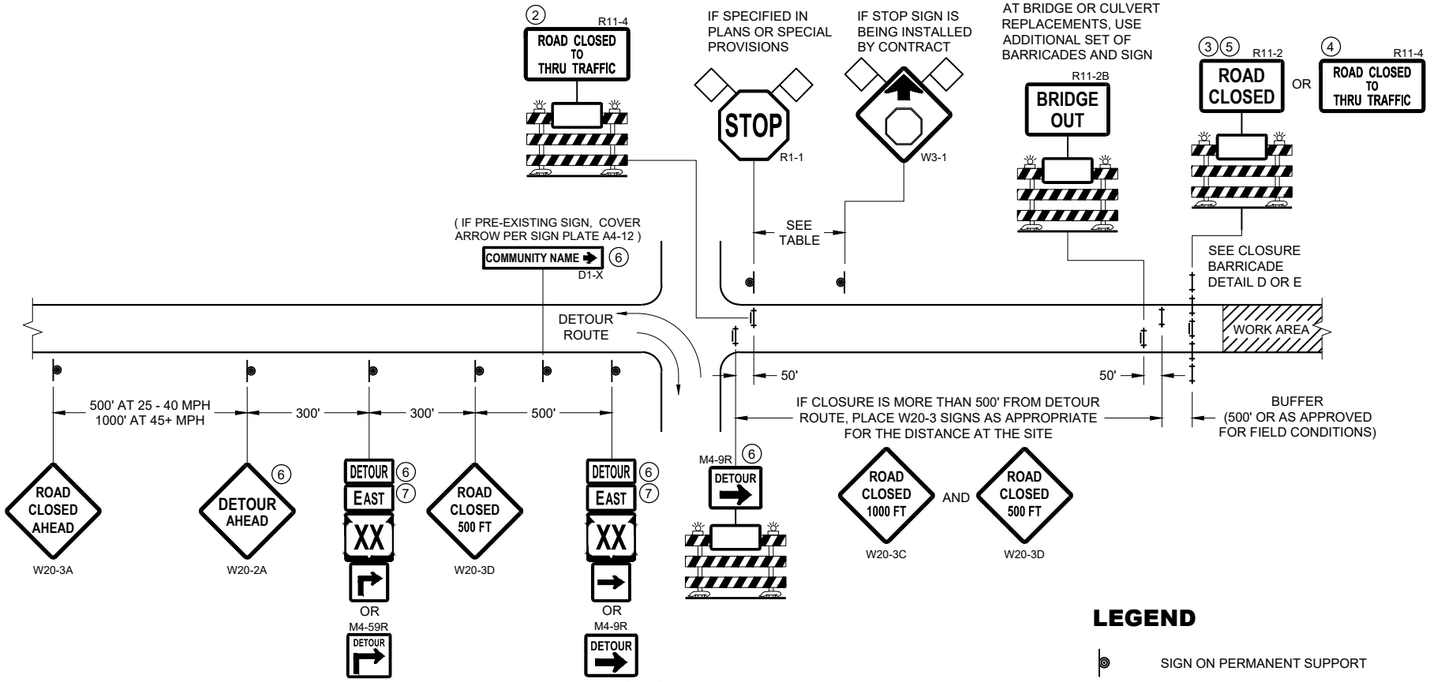
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2020
DATE

/S/ Steven Hefel
HMA PAVEMENT ENGIN

FHWA





DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR

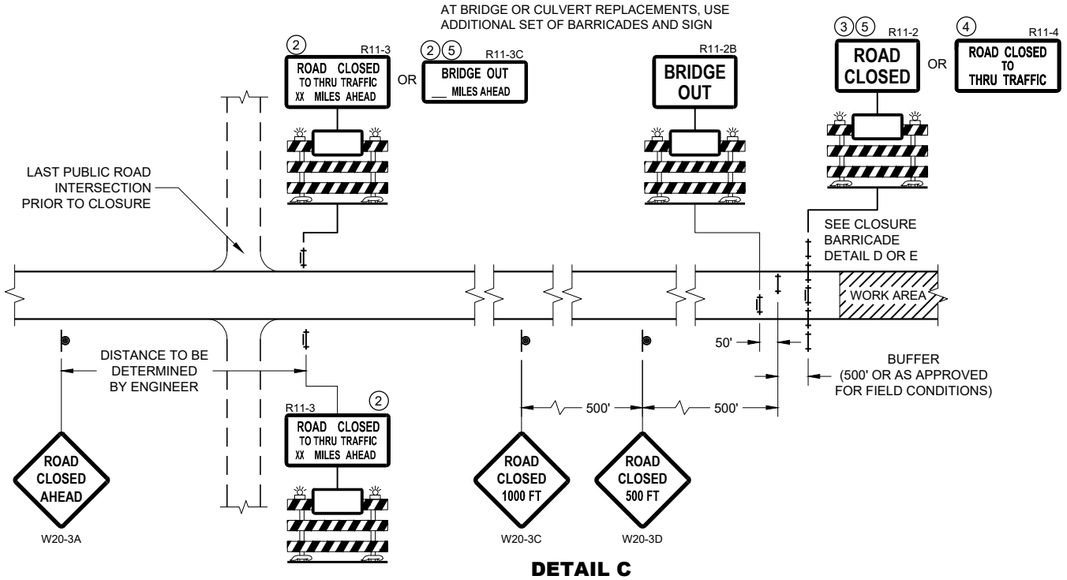
WORK ZONE GREATER THAN OR EQUAL TO ½ MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)

DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR

WORK ZONE LESS THAN ½ MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
- FLAGS, 16" X 16" MIN. (ORANGE)
- DETOUR — M4 - 8
- EAST — M3 - X
- XX — M1 - 4 OR XX — M1 - 6 OR COUNTY X — M1 - 5A
- M05 - 1 OR M06 - 1



DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

SEE SDD 15C2-SHEET "b" FOR GENERAL NOTES AND FOOTNOTES ① THROUGH ⑦

BARRICADES AND SIGNS FOR MAINLINE CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023
DATE
/S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

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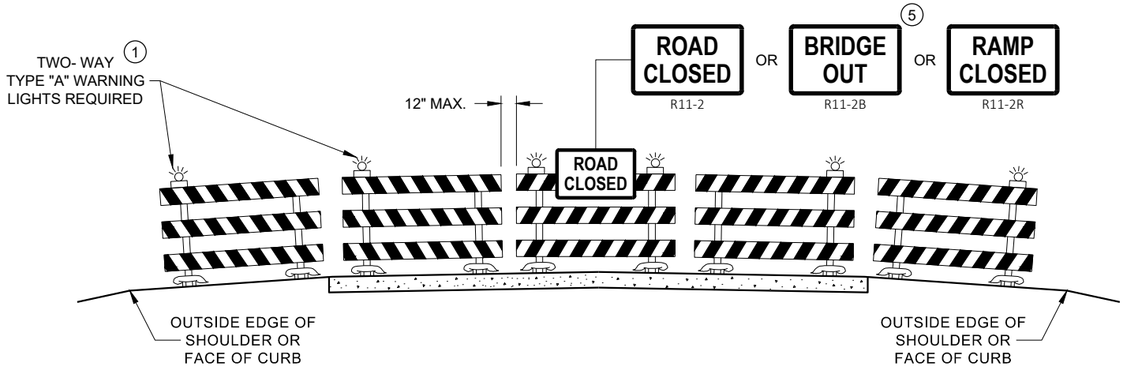
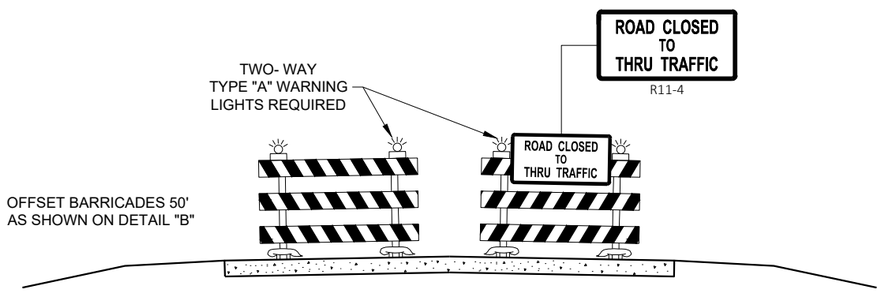
SDD 15C02 - 09a

SDD 15C02 - 09a



DETAIL D
ROAD CLOSURE BARRICADE DETAIL
APPROACH VIEW



DETAIL E
LANE CLOSURE BARRICADE DETAIL
APPROACH VIEW

SEE SDD 15C2 - SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW - INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11 - 2, R11 - 3, M4 - 9, R11 - 4, AND R10 - 61 SIGNS PLACED ON THE BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE RAIL OR BOTTOM RAILS.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
- R11 - 2 SHALL BE 48" X 30"
- R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60 " X 30"
- M4 - 9 SHALL BE 30" X 24"
- M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1 - 1 SHALL BE 36" X 36"

(1) TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT SPACING.

(2) THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.

(3) FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".

(4) FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".

(5) FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.

(6) INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.

(7) "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

BARRICADES AND SIGNS FOR VARIOUS CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023
DATE
/S/ Andrew Heidtke
WORK ZONE ENGINEER
FHWA

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SDD 15C02 - 09b

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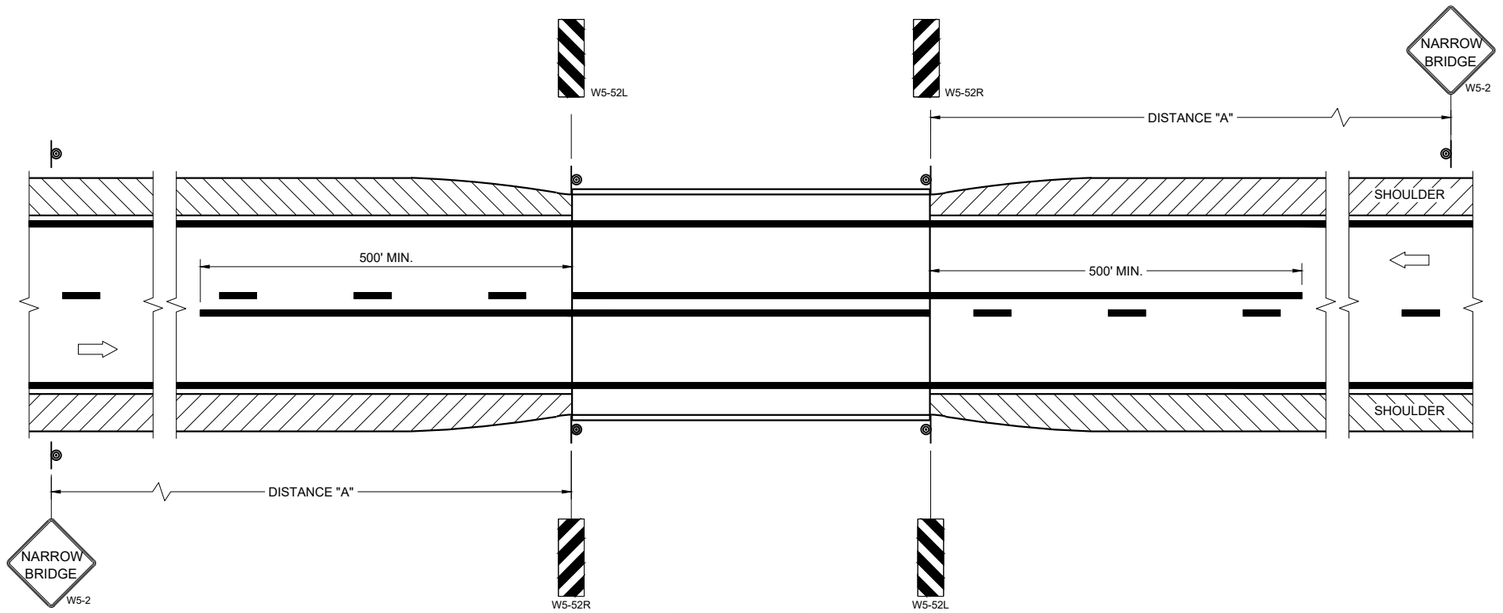
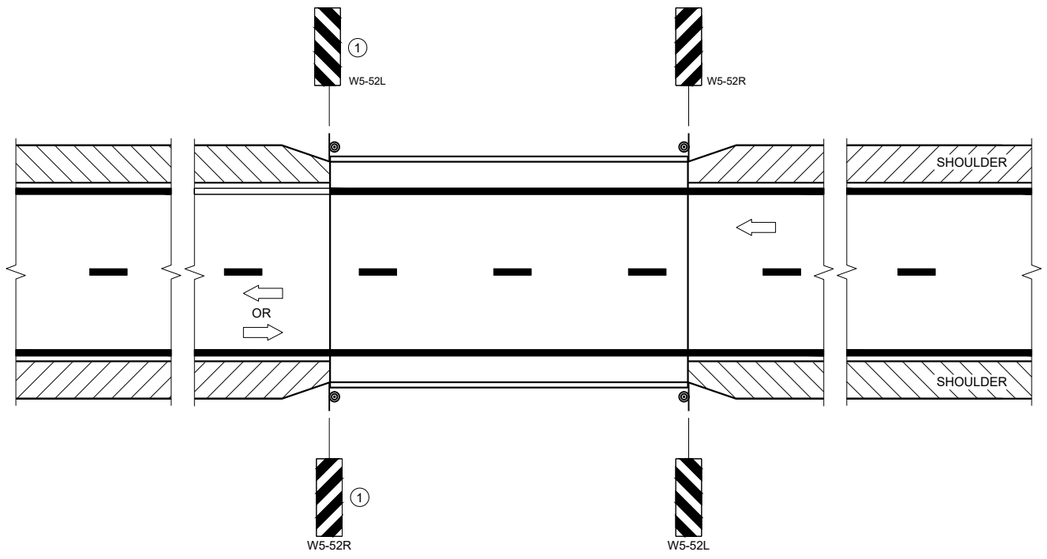
SDD 15C02 - 09b



SITUATION 1

WARRANTING CRITERIA:
BRIDGE WIDTH IS AT LEAST 16 FEET BUT LESS THAN 24 FEET.



SITUATION 2

WARRANTING CRITERIA:
1. BRIDGE WIDTH IS AT LEAST 24 FEET AND
2. BRIDGE SHOULDER WIDTH IS LESS THAN 6 FEET

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THE DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

LOCATE W5-52 SIGN POST(S) BEHIND GUARDRAIL WHEN PRESENT.

PLACE THE EDGE OF THE W5-52 SIGN IN LINE WITH FACE OF CURB OR PARAPET.

ON BRIDGE ONLY PROJECTS, PLACE 300 FEET OF EDGELINE.

OMIT EDGELINES ON ROADWAYS WITHOUT EXISTING EDGELINES.

(1) OMIT ON ONE-WAY TRAVELED WAYS.

LEGEND

SIGN ON PERMANENT SUPPORT

DIRECTION OF TRAFFIC

DISTANCE TABLE

POSTED OR 85TH PERCENTILE SPEED	DISTANCE "A"
25	150'
30	200'
35	250'
40	300'
45	400'
50	550'
55	700'

SIGNING AND MARKING FOR TWO LANE BRIDGES

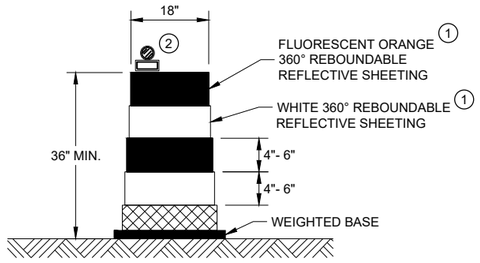
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023
DATE
/S/ Jeannie Silver
Statewide Pavement Marking Engineer
FHWA

6
SDD 15C06-12

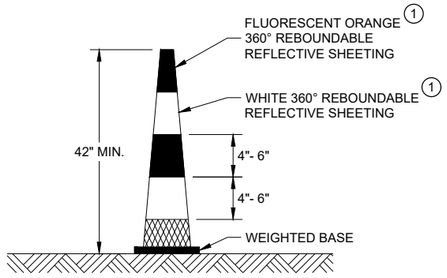
GENERAL NOTES

1. REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
2. LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.



DRUM

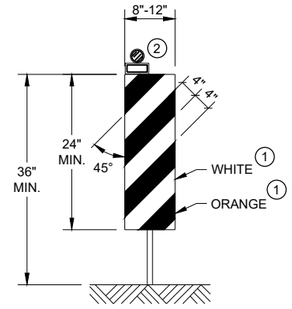
BALLAST WIDTHS
RANGE FROM 24"-36"



42" CONE

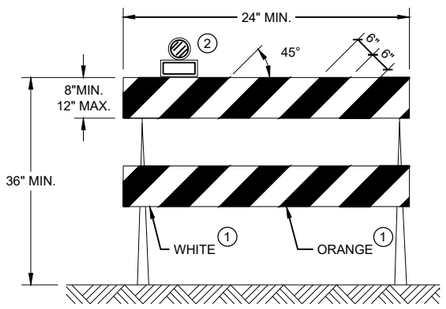
DO NOT USE IN TAPERS
½ SPACING OF DRUMS

BALLAST WIDTHS
RANGE FROM 14"-20"



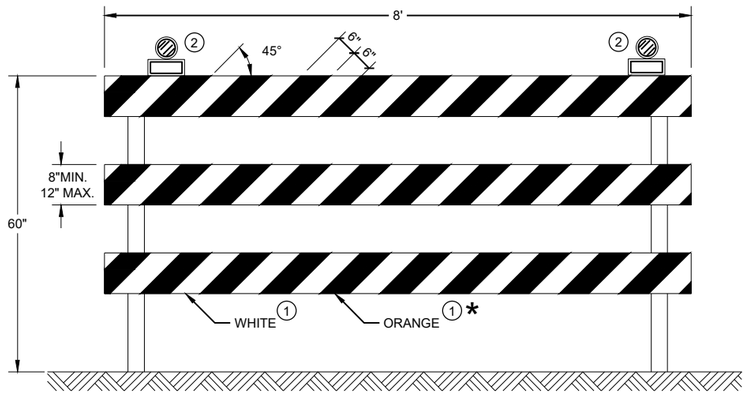
VERTICAL PANEL

THE STRIPES SHALL SLOPE DOWNWARD TO
THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE II BARRICADE

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES
MAY BE USED. ALL STRIPES SHALL SLOPE DOWNWARD
TO THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE III BARRICADE

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP
TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.

* IF USED FOR A PERMANENT APPLICATION USE RED SHEETING.

CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2022
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEE

FHWA

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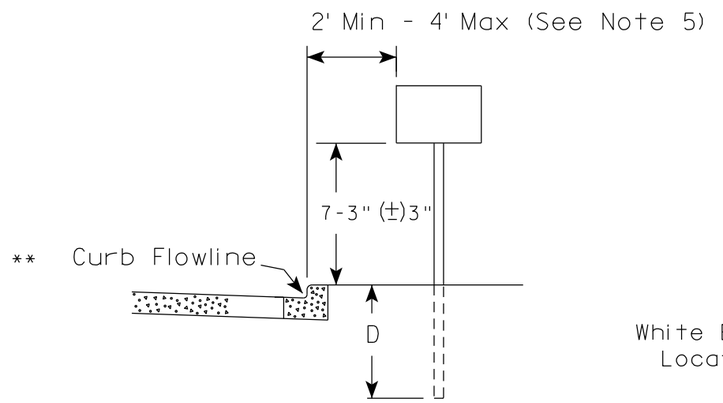
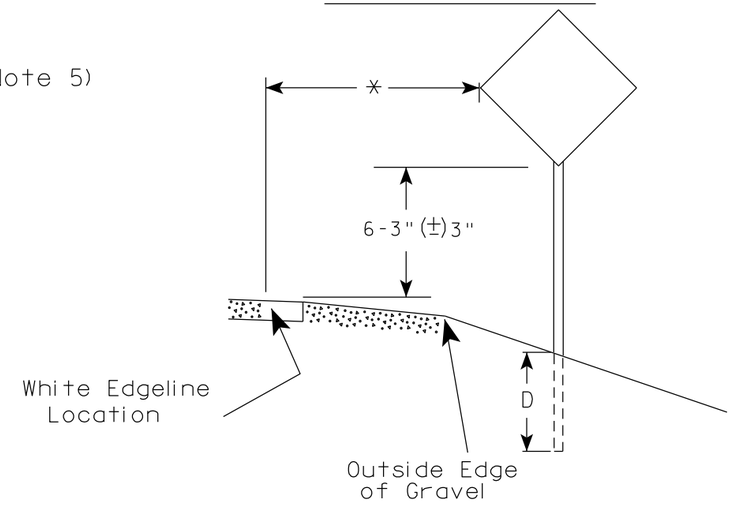
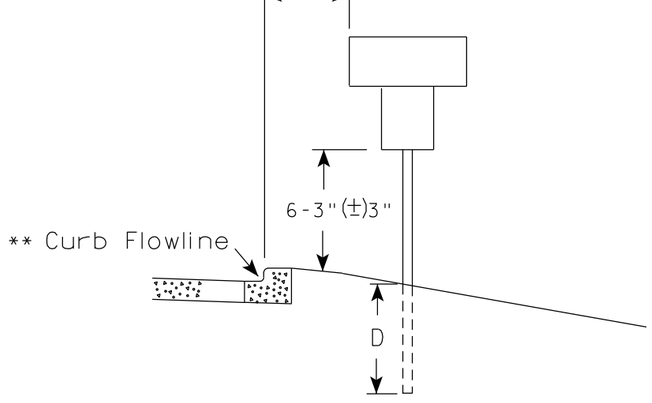
SDD 15C11 - 10b

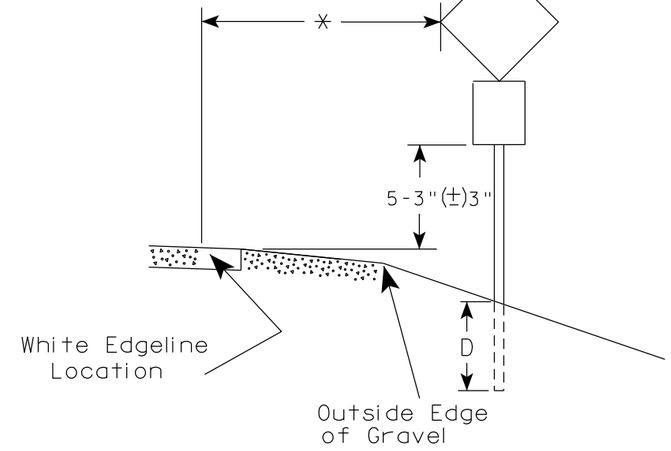
URBAN AREA



RURAL AREA (See Note 2)







** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

GENERAL NOTES

1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on or behind barrier wall, see A4-10 sign plate.
   The Double Arrow sign (W12-1D) shall be mounted at a height of 2'-3" (±) 3". The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±) 3".
3. For expressways and freeways, mounting height is 7'- 3" (±) 3" or 6'-3" (±) 3" depending upon existence of a sub-sign.
4. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±) 3".
5. Offset distance shall be consistent with existing signs or consistent throughout length of project.
6. Folding signs shall be mounted at a height of 5'-3" (±) 3" or as directd by the Engineer.

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

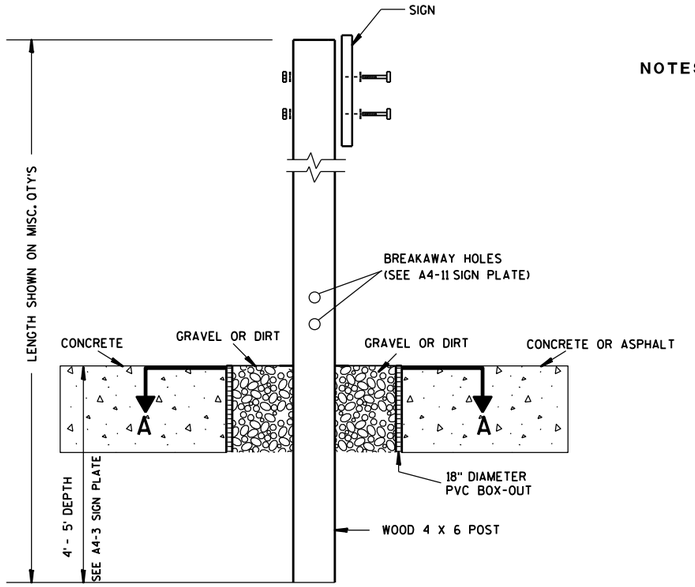
TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 12/6/23 PLATE NO. A4-3.23

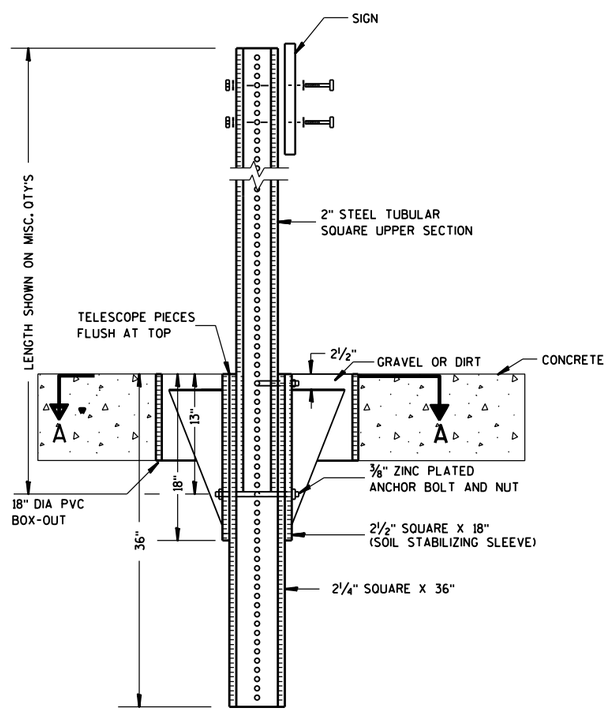
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 24	E



ELEVATION VIEW

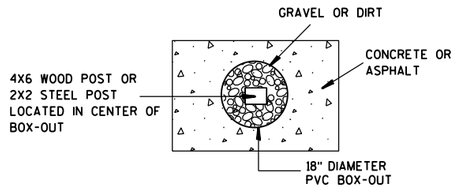
DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

NOTES:
1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



PLAN VIEW

FOR NEW CONCRETE/ASPHALT INSTALLATIONS

SIGN POST BOX-OUTS A4-3B

WISCONSIN DEPT OF TRANSPORTATION

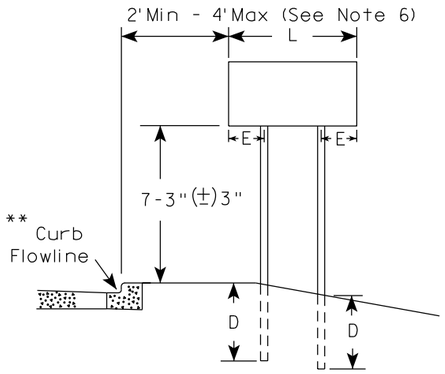
APPROVED Matthew R Rauch
for State Traffic Engineer

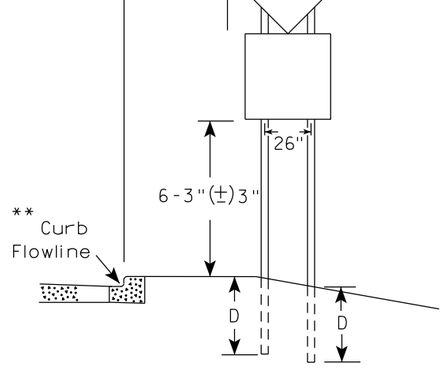
DATE 1/27/14 PLATE NO. A4-3B.1

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A43B.DGN PLOT DATE : 27-JAN-2014 09:48 PLOT BY : mscsja PLOT NAME : PLOT SCALE : 13.659812:1.000000 WISDOT/CADDS SHEET 42

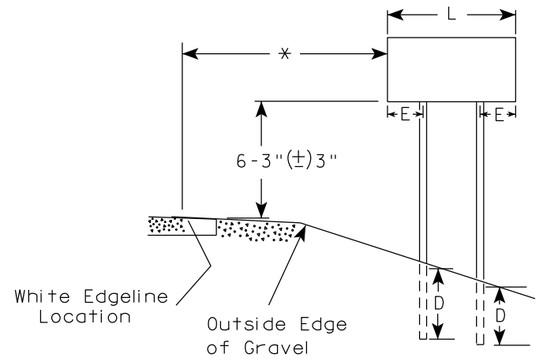
URBAN AREA

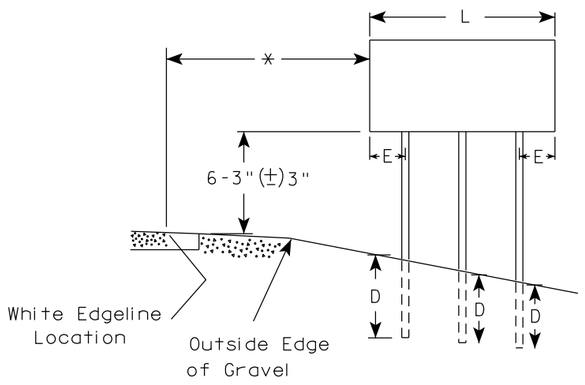


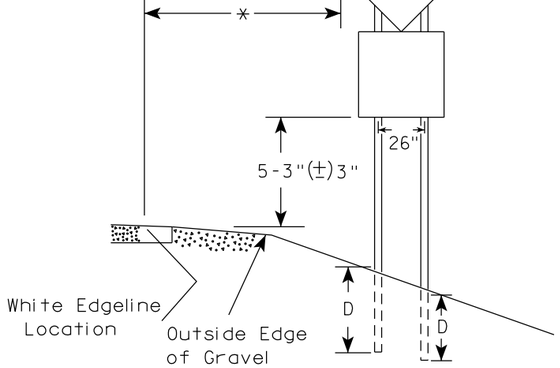


48" DIAMOND WARNING SIGN

RURAL AREA (See Note 3)







48" DIAMOND WARNING SIGN

SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)		
	L	E
***	Greater than 48" Less than 60"	12"
	60" to 108"	L/5

SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)	
L	E
Greater than 108" to 144"	12"

GENERAL NOTES

1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
2. See tables below for required number of posts.
3. For expressways and freeways, mounting height is 7'-3" (±) 3" or 6'-3" (±) 3" depending upon existence of sub-sign.
4. The (±) tolerance for mounting height is 3 inches.
5. J-Assemblies are considered to be one sign for mounting height.
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. Folding signs shall be mounted at a height of 5'-3" (±) 3" or as directed by the engineer.
8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±) 3". The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4"-3" (±) 3".

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

*** See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

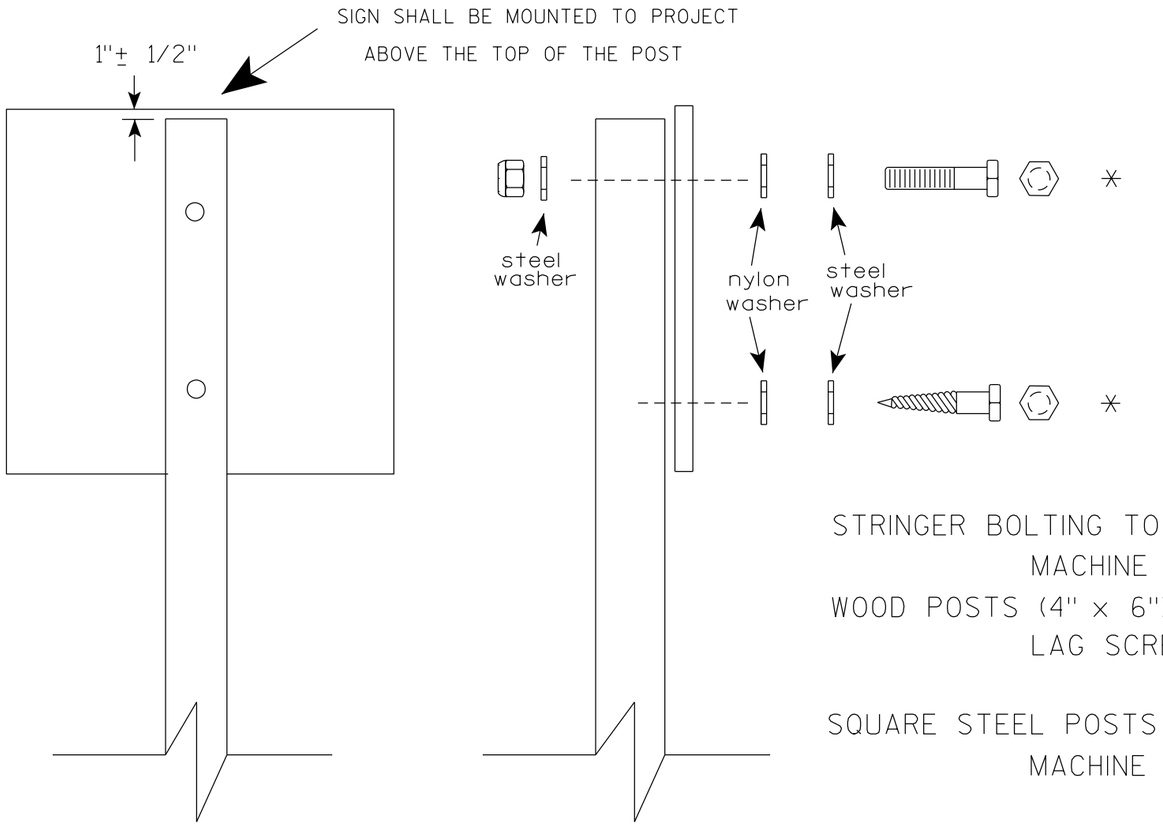
TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 12/6/23 PLATE NO. A4-4.16

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 26	E



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3

b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)

MACHINE BOLTS - 5/16" X 1-3/4" Length w/ lock nuts

WOOD POSTS (4" x 6")

LAG SCREWS - 3/8" X 3" (NO STRINGERS ON BACK OF SIGN)
3/8" X 4" (STRINGERS ON BACK OF SIGN)

SQUARE STEEL POSTS (2" x 2")

MACHINE BOLTS - 3/8" X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)
3/8" X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)

RIVETS - 9/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH

WASHERS (ALL POSTS) -

1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL

1-1/4" O.D. X 3/8" I.D. X .080 NYLON

* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS TO POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 4/1/2020 PLATE NO. A4-8.9

PROJECT NO:

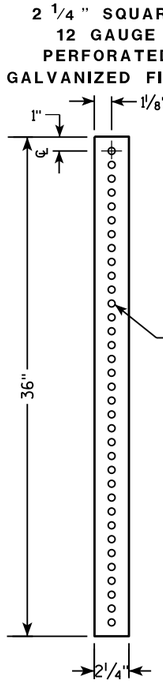
SHEET NO: 27 E

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A48.DGN PLOT DATE : 01-APRIL-2020 PLOT BY : dotc4c

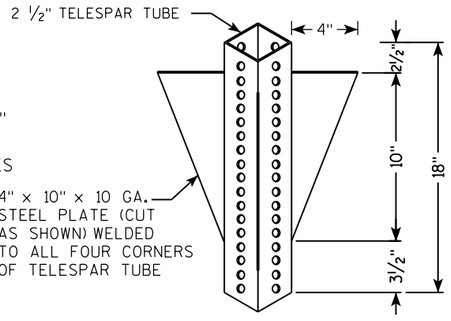
WISDOT/CADDS SHEET 42

TELESCOPIC TUBING ANCHORS
TWO PIECE SYSTEM

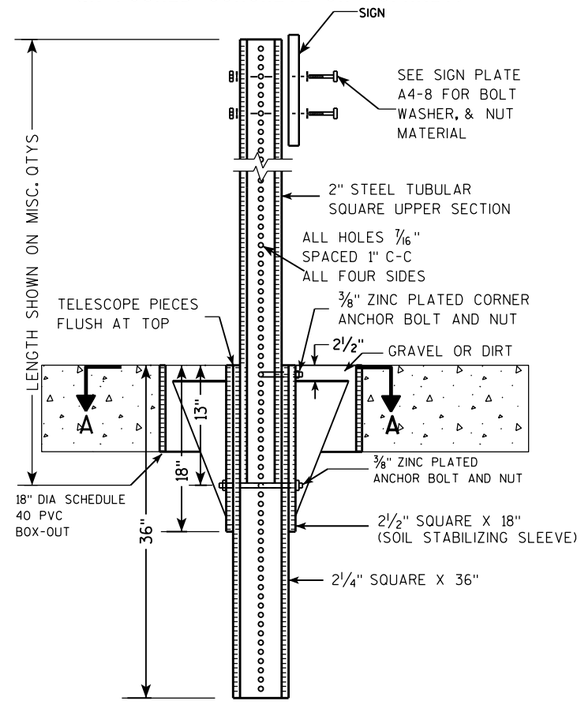
2 1/4 " SQUARE
12 GAUGE
PERFORATED
GALVANIZED FINISH



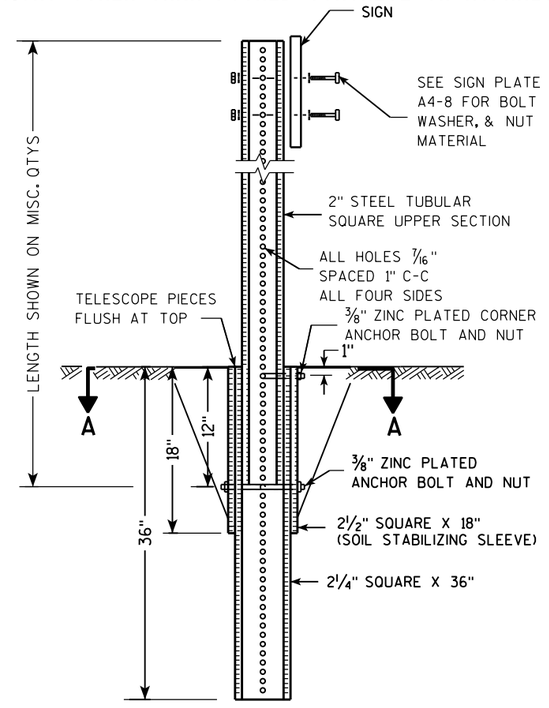
2 1/2 " SQUARE
12 GAUGE
OMNI-DIRECTIONAL
PERFORATED
SOIL STABILIZING SLEEVE
GALVANIZED FINISH

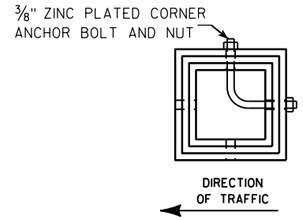


DETAIL OF TUBULAR STEEL SIGN POST
(IN POURED CONCRETE OR ASPHALT)



DETAIL OF TUBULAR STEEL SIGN POST
(IN LOCATIONS OTHER THAN POURED CONCRETE OR ASPHALT)





SECTION A-A

Area of Sign Installation (Sq. Ft.)	Number of Required Posts
9 or less	1
Greater than 9 less than or equal to 18	2
Greater than 18 less than or equal to 27	3

Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

TUBULAR STEEL
SIGN POST
A4-9

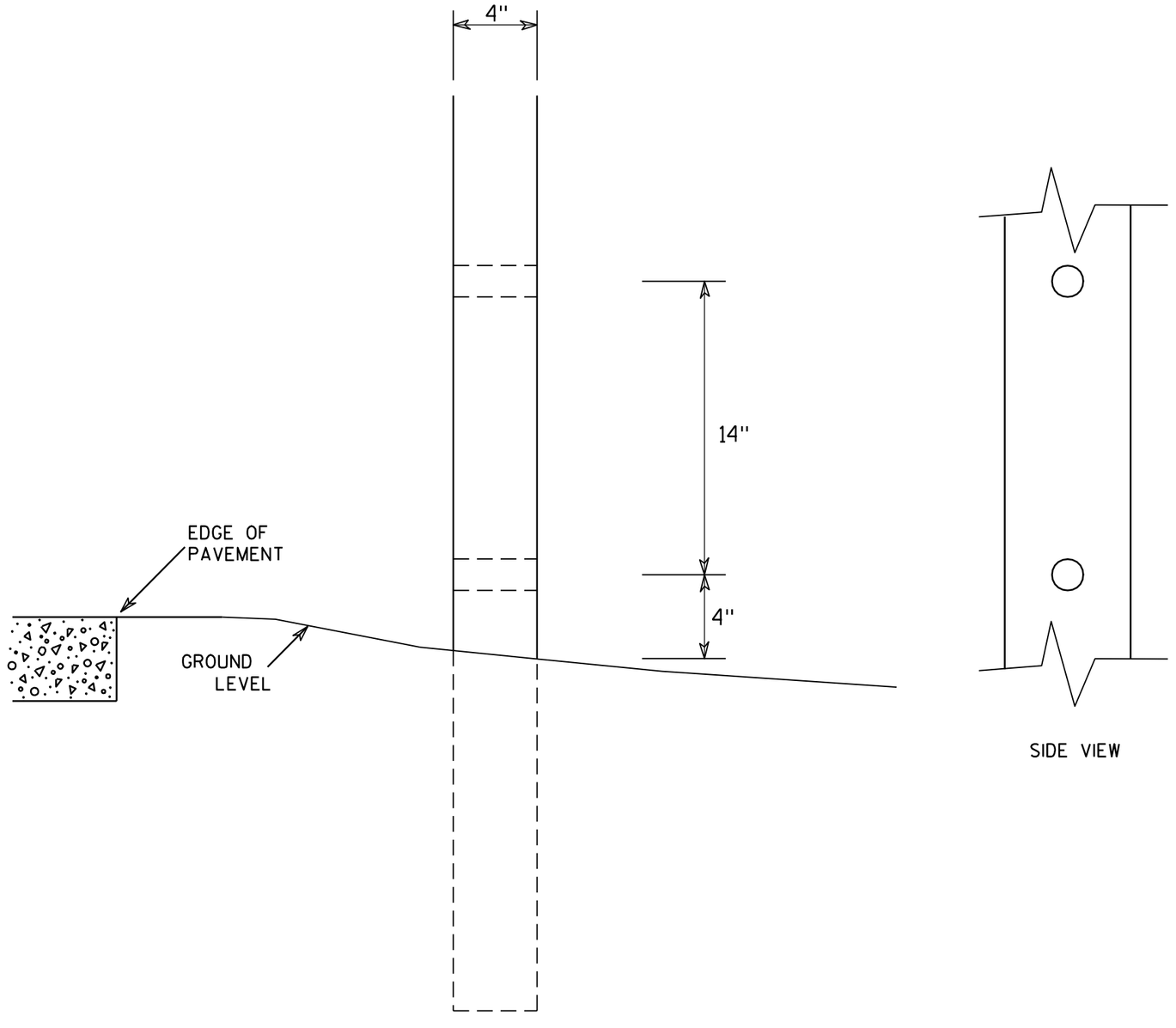
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 2/05/15 PLATE A4-9.9

PROJECT NO:	HWY:	COUNTY:		SHEET NO:	E

FILE NAME : C:\CAEFiles\Projects\tr_stdplate\A49.DGN PLOT DATE : 05-FEB-2015 17:09 PLOT BY : mscsja PLOT NAME : PLOT SCALE : 13.659812:1.000000 WISDOT/CADDS SHEET 42



SIDE VIEW

GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

7

7

4 X 6 WOOD POST MODIFICATIONS

WISCONSIN DEPT OF TRANSPORTATION

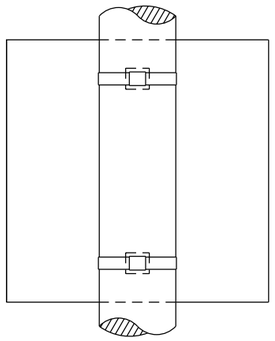
APPROVED Chester J Spang
for State Traffic Engineer

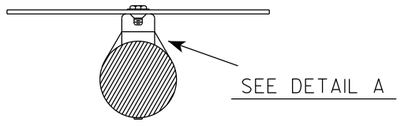
DATE 3/27/97 PLATE NO. A4-11.2

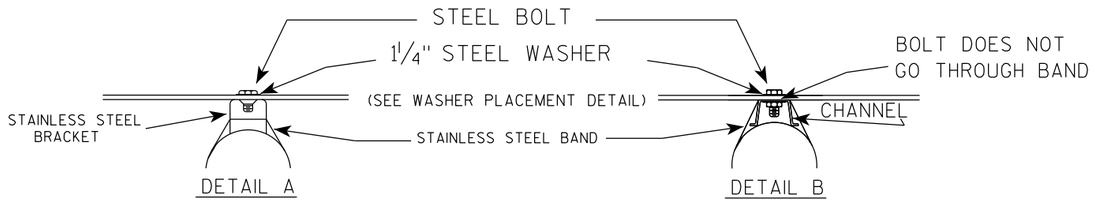
PROJECT NO:	HWY:	COUNTY:		SHEET NO: 29	E

BANDING

SINGLE SIGN



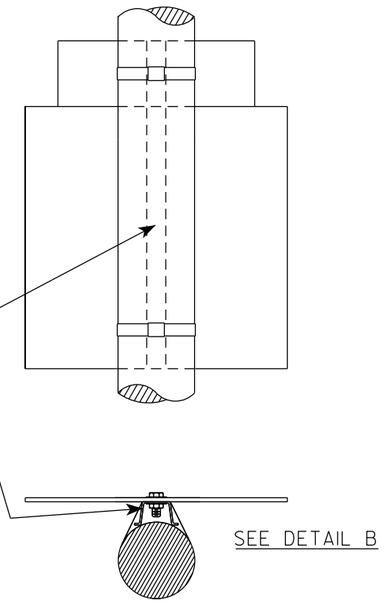




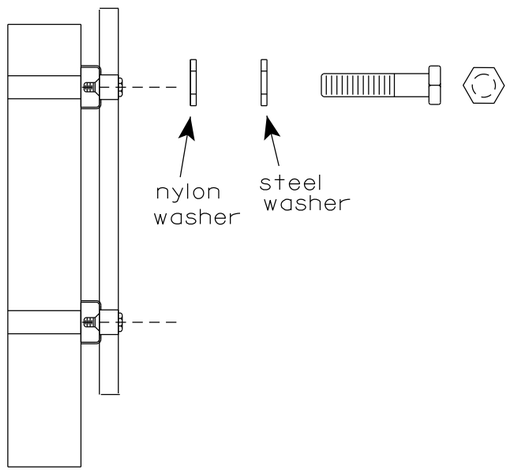
GENERAL NOTES

1. Any sign over 3 feet in width shall use the V-Block banding method. See A5-10 standard plate.
2. Signs 3 feet or greater in height shall have three bracket bands installed. Signs less than 3 feet in height shall have two bracket bands installed.
3. Banding and assembly bracket shall be stainless steel. All bands shall be ¾" in width and 0.025" thickness.
4. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
   a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
   b. Electro-galvanized in accordance with ASTM designation: B 633, Type III, SC 3

"J" ASSEMBLY



WASHER PLACEMENT



WASHERS (ALL POSTS) -
1-1/4" O.D. X ⅜" I.D. X 1/16" STEEL
1-1/4" O.D. X ⅜" I.D. X .080 NYLON
FOR ALL TYPE H SIGNS

STANDARD SIGN
SIGN BANDING DETAILS

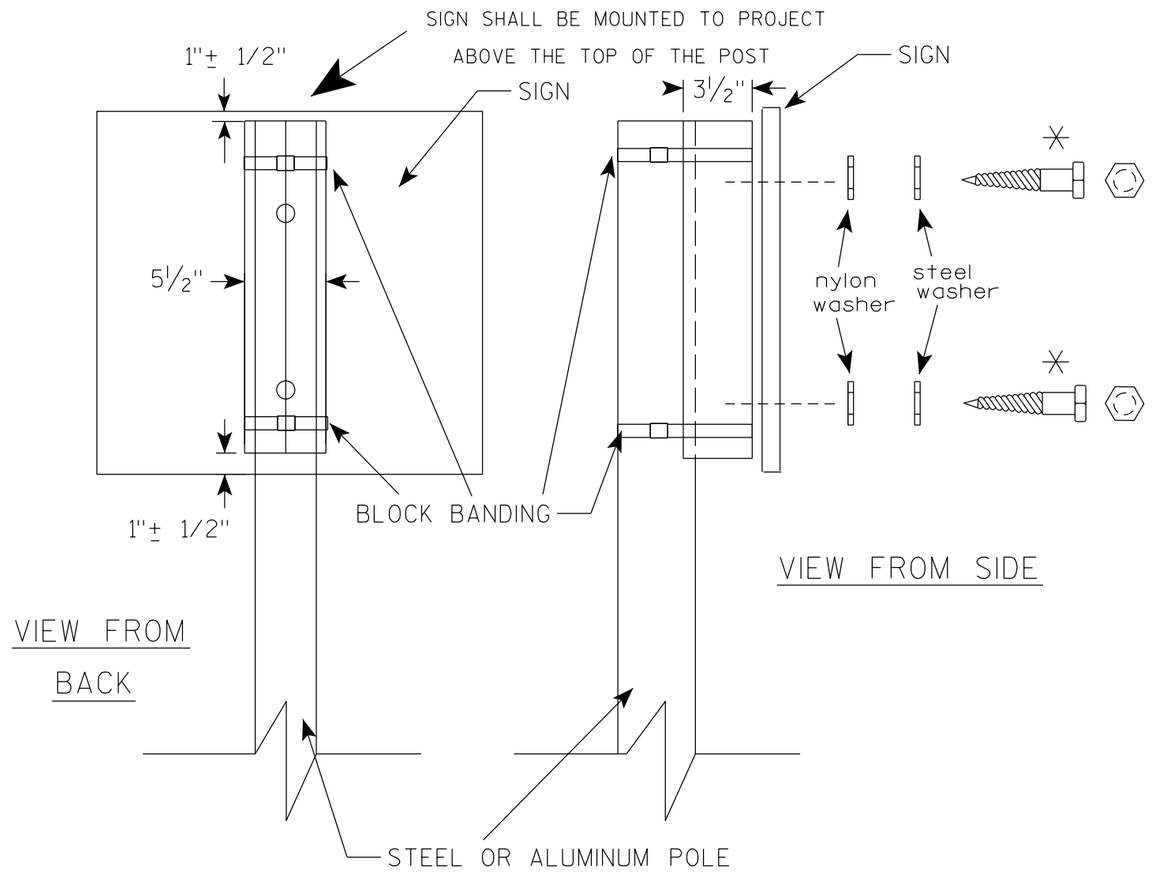
WISCONSIN DEPT OF TRANSPORTATION

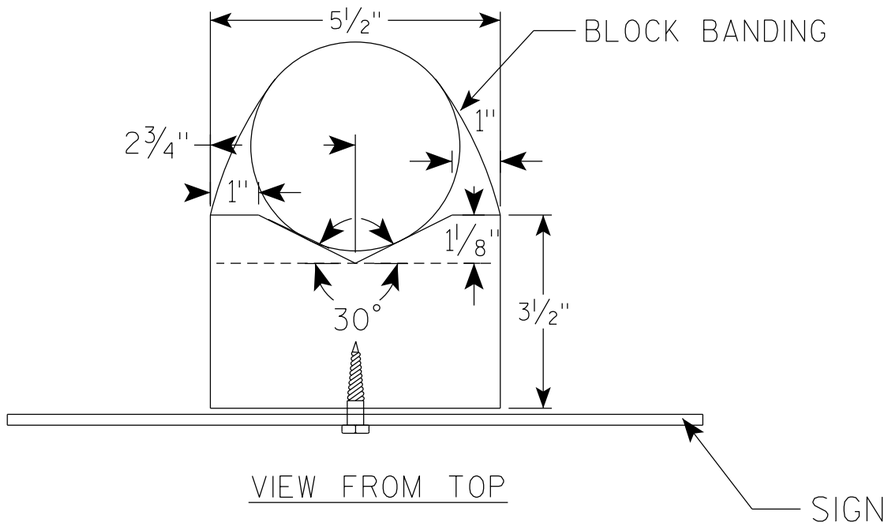
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 6/10/19 PLATE NO. A5-9.4

PROJECT NO:	HWY:	COUNTY:		SHEET NO: 30	E

FILE NAME : C:\CAEfiles\Projects\tr_stdplate\A59.dgn PLOT DATE : 10-JUN 2019 4:10 PLOT BY : mscj9h PLOT NAME : PLOT SCALE : $$.....plotscale.....$$





GENERAL NOTES

1. WOOD 4"X6" POST MATERIAL SHALL CONFORM TO 507.2.2 OF THE WisDOT STANDARD SPECIFICATIONS
2. BLOCK BANDING AND CLIPS SHALL BE STAINLESS STEEL, ¾" WIDTH AND 0.025" THICKNESS
3. SIGNS 3' OR GREATER IN HEIGHT SHALL UTILIZE 3 BLOCK BANDS. SIGNS UNDER 3' IN HEIGHT SHALL UTILIZE 2 BLOCK BANDS
4. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA, BUT NORNALLY THERE ARE TWO. FOR SIGNS GREATER THAN 9 S.F. 3 FASTENERS SHALL BE USED.
5. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
   a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
   b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3
6. ALL BOLTS SHALL HAVE HEXAGONAL HEADS.
7. STEEL WASHERS SHALL BE 1¼" O.D. X ⅜" I.D. X 1/16"
8. NYLON WASHERS SHALL BE 1¼" O.D. X ⅜" I.D. X .080 FOR TYPE H OR TYPE F FACE SIGN

✳ LAG BOLTS SHALL BE ⅜" X 2½"

BLOCK BANDING DETAIL (V-BLOCK OPTION)

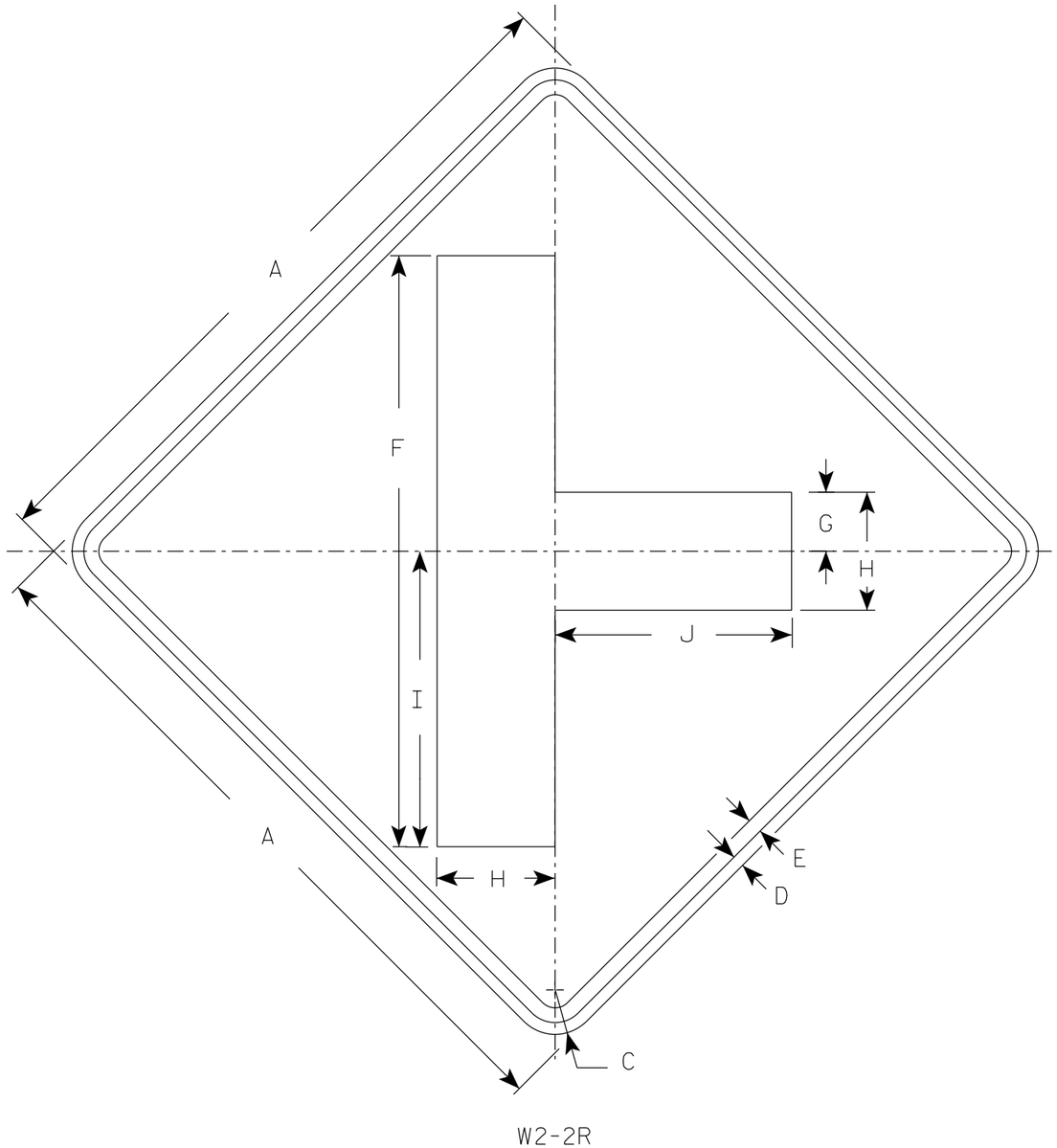
WISCONSIN DEPT OF TRANSPORTATION

APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 4/19/2022 PLATE NO. A5-10.3

PROJECT NO:

SHEET NO: 31 E



W2-2R

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
   Background - Yellow
   Message - Black
3. W2-2L same as W2-2R but is rotated 180° when mounted.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/2	3/8	1/2	20	2	4	10	8																	4.0
2S	30		1 7/8	1/2	5/8	25	2 1/2	5	12 1/2	10																	6.25
2M	30		1 7/8	1/2	5/8	25	2 1/2	5	12 1/2	10																	6.25
3	36		2 1/4	5/8	3/4	30	3	6	15	12																	9.0
4	48		3	3/4	1	40	4	8	20	16																	16.0
5																											

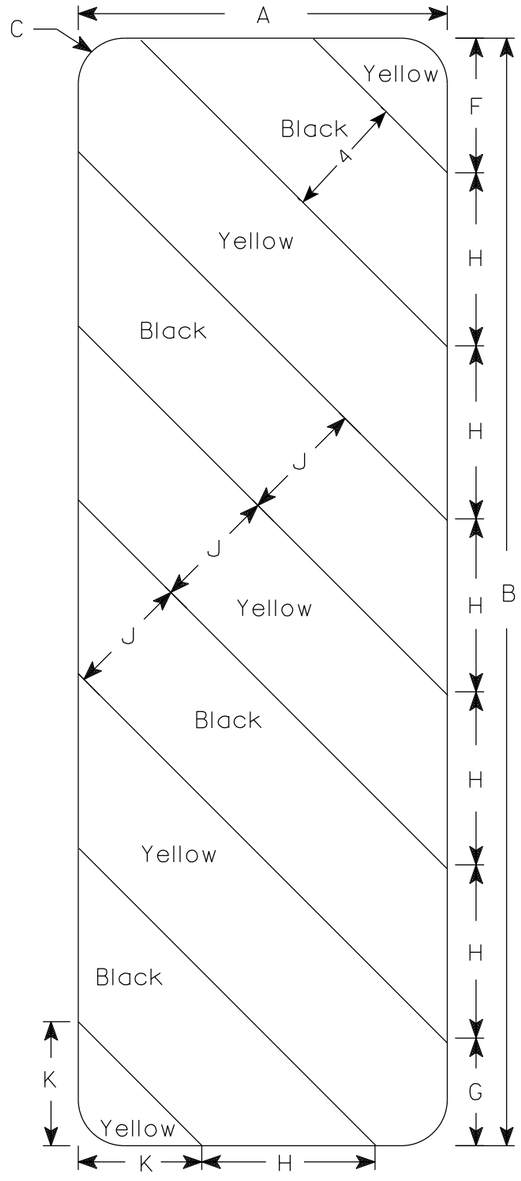
STANDARD SIGN
W2-2 L&R

WISCONSIN DEPT OF TRANSPORTATION

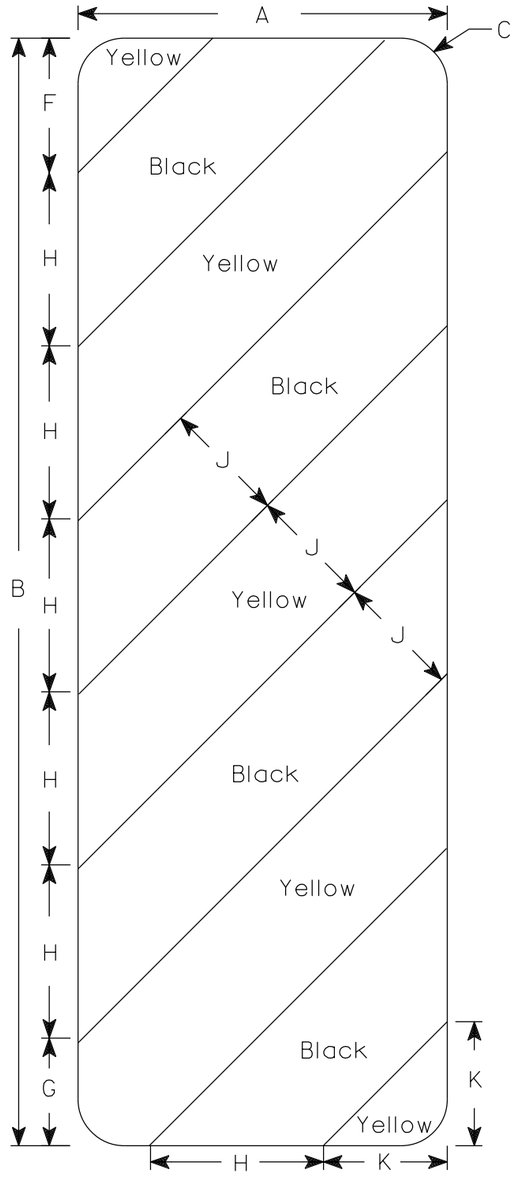
APPROVED Matthew R Rauch
For State Traffic Engineer

DATE 8/16/2023 PLATE NO. W2-2.8

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 32 | E



W5-52L



W5-52R

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
   Background - Yellow
   Message - Black
3. Alternate colors of stripes as shown.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	12	36	1 1/2			4 3/8	3 1/2	5 5/8	45°	4	4																3.0
2M	12	36	1 1/2			4 3/8	3 1/2	5 5/8	45°	4	4																3.0
3	18	54	1 1/2			6	5 1/2	8 1/2	45°	6	6 9/16																6.75
4																											
5																											

STANDARD SIGN

W5-52L & W5-52R

WISCONSIN DEPT OF TRANSPORTATION

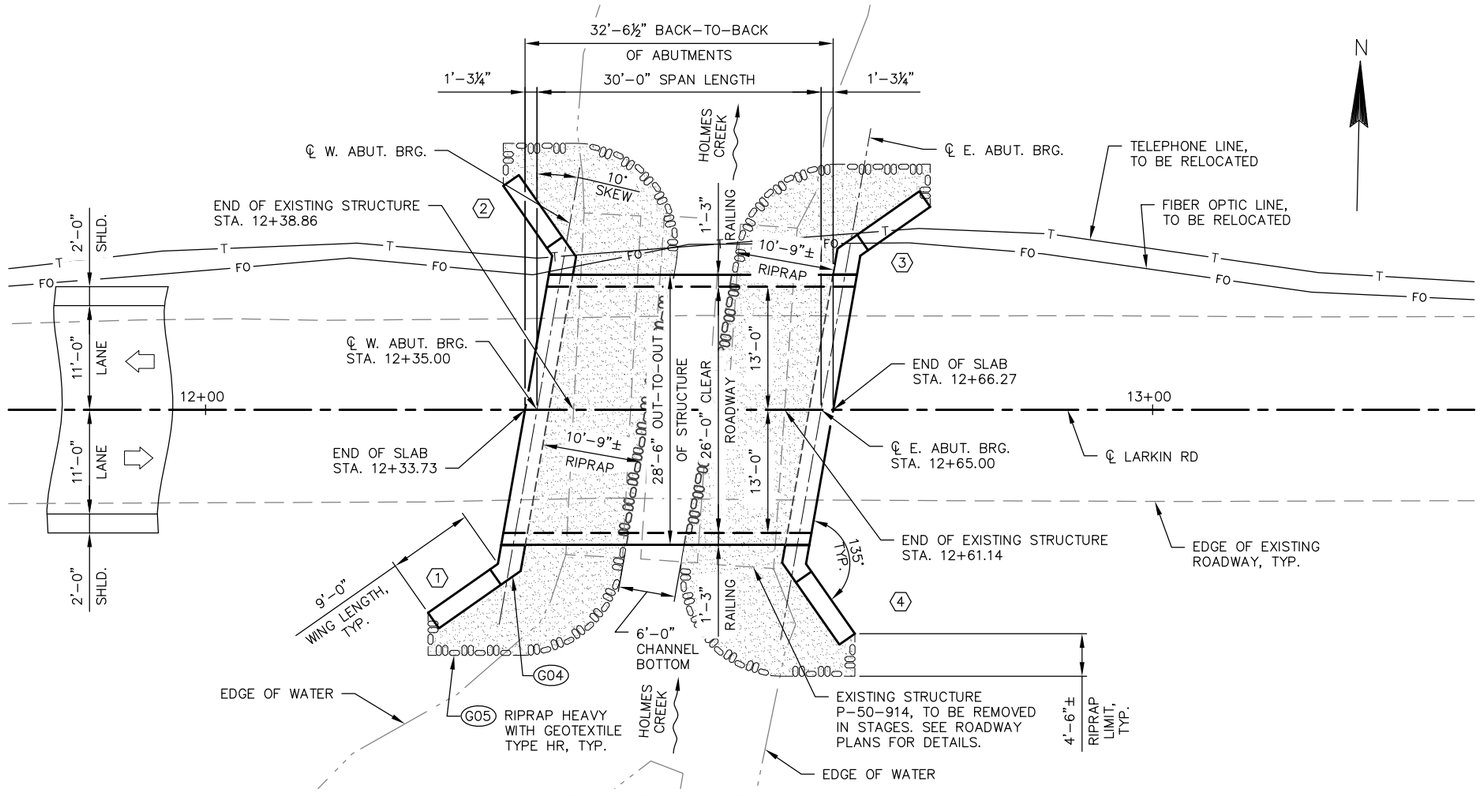
APPROVED Matthew R Rauch
for State Traffic Engineer

DATE 3/4/2024 PLATE NO. W5-52.10

PROJECT NO: | HWY: | COUNTY: | SHEET NO: 33 E

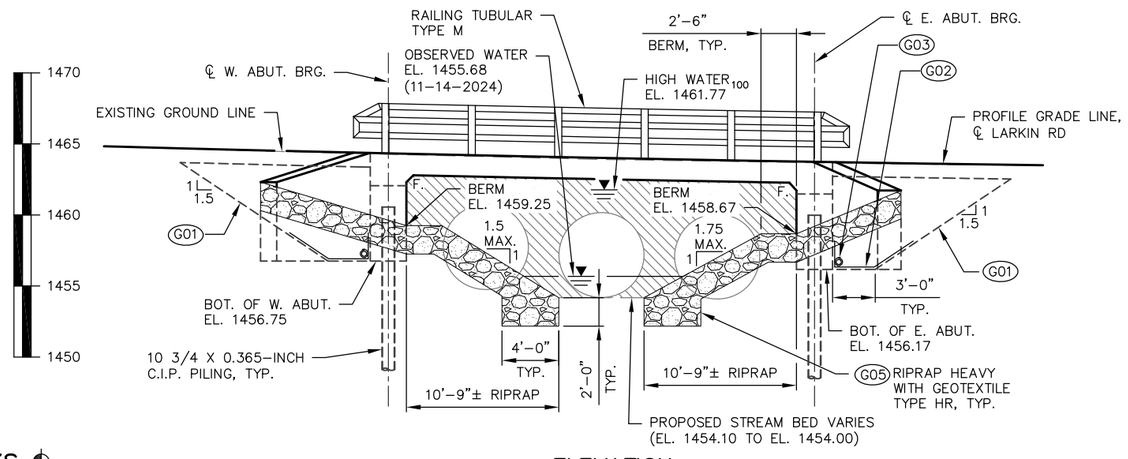
7

7



PLAN B-50-97

(SINGLE SPAN CONCRETE FLAT SLAB BRIDGE)



ELEVATION

(THRU HOLMES CREEK, LOOKING NORTH)

BENCH MARKS

NO.	STATION/OFFSET	DESCRIPTION	ELEVATION
BM #1	N/A	RAILROAD SPIKE IN POWER POLE	1483.62
BM #2	10+11.04, 169' LT.	RAILROAD SPIKE IN POWER POLE	1458.62
BM #3	15+13.01, 30' RT.	BARN SPIKE IN POWER POLE	1464.36

HORIZONTAL DATUM AND ADJUSTMENT: NAD 83 (2011)
VERTICAL DATUM AND ADJUSTMENT: NAVD 88 (2012)
COORDINATE REFERENCE SYSTEM: WISCRS PRICE CO.

TRAFFIC DATA:

LARKIN RD
A.A.D.T. (2026) ——— 58
A.A.D.T. (2046) ——— 65
DESIGN SPEED ——— 60 M.P.H.

BRIDGE OFFICE CONTACT
AARON BONK, P.E.
(608) 261-0261

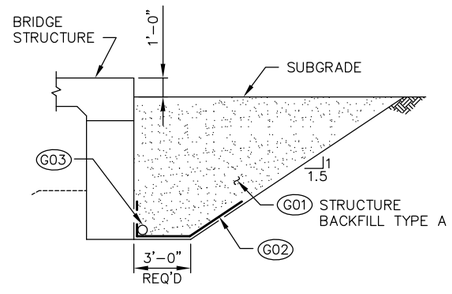
CONSULTANT CONTACT
ANDY KNUTSON, P.E., S.E.
(608) 588-7866

NOTES

- EXCAVATION AS INDICATED IN THE HATCH AREAS, TO BE INCLUDED IN THE BID ITEM "EXCAVATION FOR STRUCTURES BRIDGES B-50-97".
- (G01) BACKFILL PAY LIMITS. BACKFILL BEYOND BACKFILL PAY LIMITS SHALL BE INCLUDED WITH BID ITEM "EXCAVATION FOR STRUCTURES BRIDGES B-50-97". LIMITS OF EXCAVATION SHALL BE DETERMINED BY THE CONTRACTOR.
- (G02) "GEOTEXTILE TYPE DF SCHEDULE A" LIMITS. EXTEND 2'-0" ABOVE BOTTOM OF ABUTMENT FOR THE ENTIRE ABUTMENT BODY LENGTH.
- (G03) PIPE UNDERDRAIN WRAPPED (6-INCH). SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. ATTACH RODENT SHIELD AT ENDS OF PIPE UNDERDRAIN AS DETAILED IN "ABUTMENT DETAILS" SHEET.
- (G04) NAME PLATE REQUIRED AND BENCH MARK CAP (WHEN SUPPLIED). FOR LOCATION SEE "ABUTMENTS" SHEET.
- (G05) FILL VOIDS IN RIPRAP HEAVY WITH INFILL RIPRAP. SEE ROADWAY PLANS FOR DETAILS, QUANTITIES AND BID ITEM.
- ⬡ INDICATES WING NUMBER

LIST OF DRAWINGS

1. GENERAL PLAN
2. CROSS SECTION, GENERAL NOTES & QUANTITIES
3. SUBSURFACE EXPLORATION
4. ABUTMENTS
5. ABUTMENT DETAILS
6. SUPERSTRUCTURE
7. SUPERSTRUCTURE DETAILS
8. RAILING TUBULAR TYPE M



ABUTMENT BACKFILL DETAIL

STATE PROJECT NUMBER

8724-04-71

DESIGN DATA

LIVE LOAD:

DESIGN LOADING ——— HL-93
INVENTORY RATING FACTOR ——— RF=1.17
OPERATING RATING FACTOR ——— RF=1.51
WISCONSIN STANDARD PERMIT VEHICLE RATING (WIS.-SPV): ——— 250 KIPS

STRUCTURE IS DESIGNED FOR A FUTURE WEARING SURFACE OF 20 POUNDS PER SQUARE FOOT.

MATERIAL PROPERTIES:

CONCRETE MASONRY, SLAB ——— $f'c$ = 4,000 P.S.I.
ALL OTHER ——— $f'c$ = 3,500 P.S.I.
HIGH-STRENGTH BAR STEEL REINFORCEMENT ——— f_y = 60,000 P.S.I.

FOUNDATION DATA:

ABUTMENTS TO BE SUPPORTED ON 10 3/4 X 0.365-INCH C.I.P. PILING DRIVEN TO A REQUIRED DRIVING RESISTANCE OF 120 TONS PER PILE** AT W. ABUT. AND 120 TONS PER PILE** AT E. ABUT. AS DETERMINED BY THE MODIFIED GATES DYNAMIC FORMULA. ESTIMATED 40 FT PILE LENGTHS AT W. ABUT. AND 40 FT PILE LENGTHS AT E. ABUT.

**THE FACTORED AXIAL RESISTANCE OF PILES IN COMPRESSION USED FOR DESIGN IS THE REQUIRED DRIVING RESISTANCE MULTIPLIED BY A RESISTANCE FACTOR OF 0.5 USING MODIFIED GATES DYNAMIC FORMULA TO DETERMINE DRIVEN PILE CAPACITY.

HYDRAULIC DATA:

100 YEAR DESIGN FREQUENCY:
Q_{100} ——— 840 C.F.S.
Q_{100} (THRU BRIDGE) ——— 840 C.F.S.
DRAINAGE AREA ——— 15.4 SQ. MI.
BRIDGE WATER AREA ——— 84 SQ. FT.
BRIDGE VELOCITY ——— 9.98 F.P.S.
HIGH WATER100 EL. ——— 1461.77 FT.
SCOUR CRITICAL CODE ——— 5
Q_2 ——— 209 C.F.S.
Q_2 ELEVATION ——— 1458.10 FT.
Q_2 VELOCITY ——— 5.71 F.P.S.

NO.	DATE	REVISION	BY

WESTBROOK Associated Engineers, Inc.
619 EAST HOXIE STREET
P.O. BOX 429
SPRING GREEN, WI 53588
PHONE (608) 588-7866
FAX (608) 588-7954

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

ACCEPTED JLR 12/02/25
CHIEF STRUCTURES DESIGN ENGINEER — DATE

STRUCTURE B-50-97

LARKIN RD OVER HOLMES CREEK

COUNTY	TOWN/~~CITY/VILLAGE~~
PRICE	OGEMA

DESIGN SPEC. AASHTO LRFD DESIGN SPEC.

DESIGNED BY	DESIGN CK'D.	DRAWN BY	PLANS CK'D.
JDO	CDS	JDO	ACK

GENERAL PLAN

SHEET 1 OF 8

34

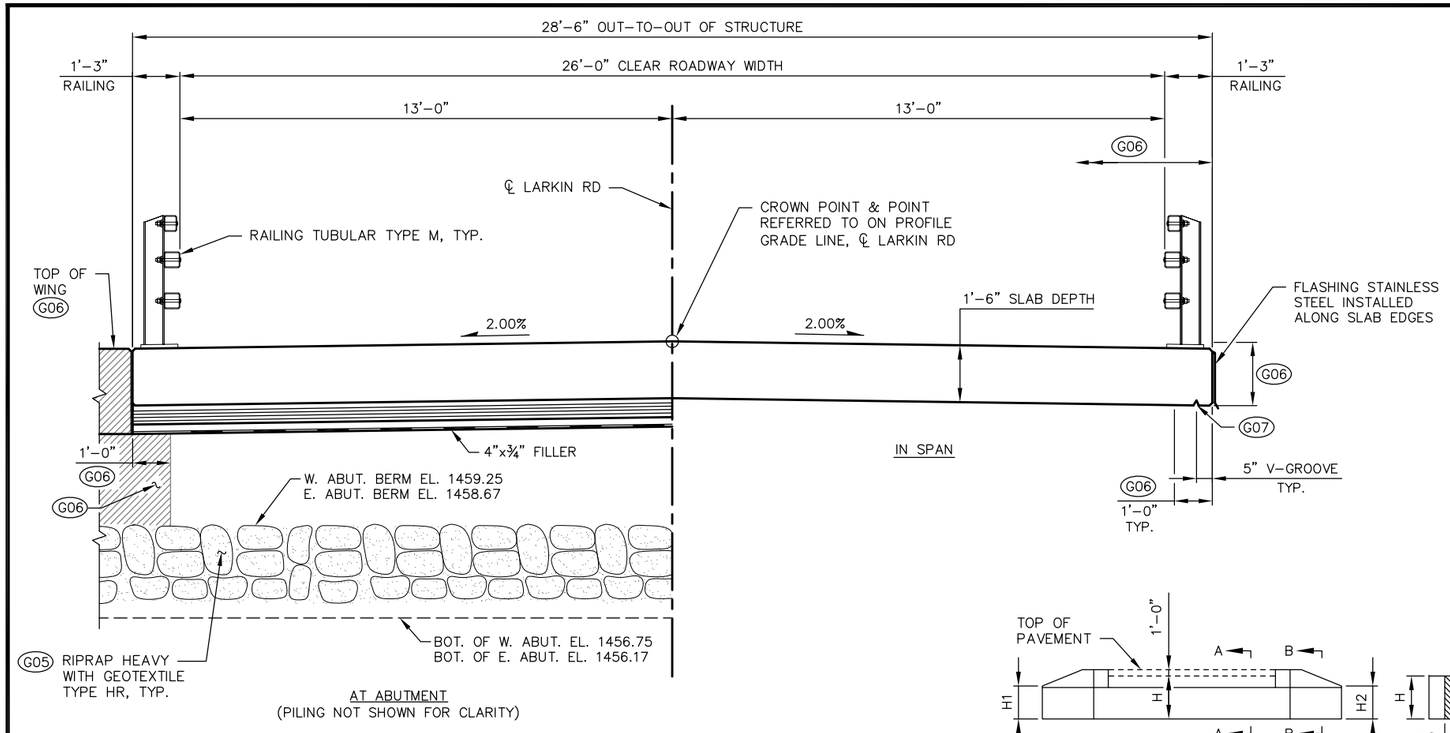
8

8

FILE: B-50-0097_01_gen.dwg
PLOT SCALE:

I.D. 8724-04-71

PLOT DATE: Sep 22, 2025



AT ABUTMENT
(PILING NOT SHOWN FOR CLARITY)

CROSS SECTION THRU ROADWAY

(LOOKING EAST)

NOTES

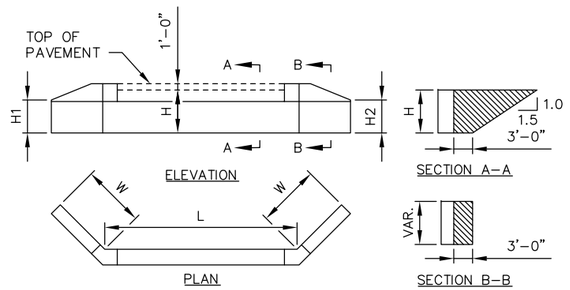
(G05) FILL VOIDS IN RIPRAP HEAVY WITH INFILL RIPRAP. SEE ROADWAY PLANS FOR DETAILS, QUANTITIES AND BID ITEM.

(G06) COAT WITH "PROTECTIVE SURFACE TREATMENT" AS PER THE STANDARD SPECIFICATIONS. PROTECTIVE SURFACE TREATMENT TO BE APPLIED TO ENTIRE EXPOSED TOP OF SLAB, INCLUDING THE SLAB EDGES AND 1'-0" UNDER THE SLAB, THE TOP AND EXTERIOR EXPOSED FACE OF WINGS, AND THE FRONT FACE OF THE ABUTMENTS TO 1'-0" PAST THE EDGE OF SLAB.

(G07) ¾" V-GROOVE REQ'D. EXTEND V-GROOVE TO 6" FROM FRONT FACE OF ABUTMENTS.

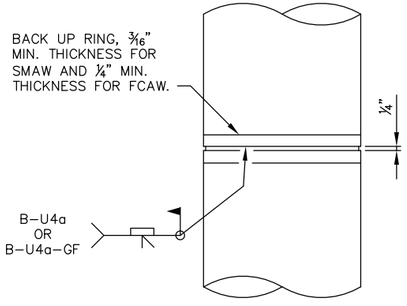
TOTAL ESTIMATED QUANTITIES

ITEM NO.	BID ITEMS	UNIT	W. ABUT.	E. ABUT.	SUPER.	TOTALS
203.0220	REMOVING STRUCTURE P-50-914	EACH	---	---	---	1
206.1001	EXCAVATION FOR STRUCTURES BRIDGES B-50-97	EACH	---	---	---	1
210.1500	BACKFILL STRUCTURE TYPE A	TON	150	150	---	300
502.0100	CONCRETE MASONRY BRIDGES	CY	25.9	25.9	56.0	108
502.3200	PROTECTIVE SURFACE TREATMENT	SY	15	15	119	149
505.0400	BAR STEEL REINFORCEMENT HS STRUCTURES	LB	2125	2125	---	4250
505.0600	BAR STEEL REINFORCEMENT HS COATED STRUCTURES	LB	1405	1405	10210	13020
513.4061	RAILING TUBULAR TYPE M	LF	---	---	70	70
516.0500	RUBBERIZED MEMBRANE WATERPROOFING	SY	6	6	---	12
550.0500	PILE POINTS	EACH	7	7	---	14
550.2106	PILING CIP CONCRETE 10 3/4 X 0.365-INCH	LF	280	280	---	560
606.0300	RIPRAP HEAVY	CY	65	65	---	130
612.0406	PIPE UNDERDRAIN WRAPPED 6-INCH	LF	80	80	---	160
645.0111	GEOTEXTILE TYPE DF SCHEDULE A	SY	32	32	---	64
645.0120	GEOTEXTILE TYPE HR	SY	105	105	---	210
SPV.0090.01	FLASHING STAINLESS STEEL	LF	---	---	55	55
(NON-BID ITEM)	FILLER	SIZE				½" & ¾"



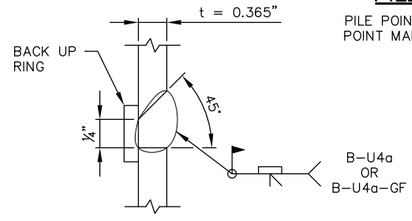
ABUTMENT BACKFILL DIAGRAM

L = ABUTMENT BODY LENGTH AT BACKFACE (FT)
H = AVERAGE ABUTMENT FILL HEIGHT (FT)
H1 = WING 1 HEIGHT AT TIP (FT)
H2 = WING 2 HEIGHT AT TIP (FT)
W = WING LENGTH (FT)
EF = EXPANSION FACTOR (1.20 FOR CY BID ITEMS AND 1.00 FOR TON BID ITEMS)
V_{CF} = (L)(3.0')(H) + (L)(0.5)(1.5H)(H) + (3')(0.5)(H1+H2+H+H)(W)
V_{CY} = V_{CF}(EF)/27
V_{TON} = V_{CY}(2.0)



CAST-IN-PLACE 'PIPE PILE'

NOTE: CAST-IN-PLACE PILE SHELL MATERIAL SHALL BE IN ACCORDANCE WITH THE STANDARD SPECIFICATION.



C.I.P. PILE WELD DETAIL

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.

BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2" CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE.

BEVEL EXPOSED EDGES OF CONCRETE ¾" UNLESS OTHERWISE NOTED.

THE SLOPE OF THE FILL IN FRONT OF THE ABUTMENTS SHALL BE COVERED WITH HEAVY RIPRAP AND GEOTEXTILE TYPE HR TO THE EXTENT SHOWN ON THE "GENERAL PLAN" SHEET AND THE ABUTMENT SHEETS.

THE BACKFILL QUANTITIES ARE BASED ON THE PAY LIMITS SHOWN ON THE PLANS AND MAY NOT REFLECT ACTUAL PLACED QUANTITIES. "BACKFILL STRUCTURE TYPE A" REQUIRED DIRECTLY BEHIND ABUTMENTS AND ABUTMENT WING FOR 3 FEET. BACKFILL PLACED BEYOND PAY LIMITS OR EXCEEDING PLAN QUANTITIES SHALL BE INCLUDED WITH "EXCAVATION FOR STRUCTURES BRIDGES B-50-97".

EXCAVATION BELOW THE ABUTMENT AND ABUTMENT BEDDING MATERIALS REQUIRES ENGINEER APPROVAL. GEOTEXTILE SHALL BE SET AT THE BOTTOM OF EXCAVATION AND EXTEND 2'-0" ABOVE BOTTOM OF ABUTMENT.

THE UPPER LIMITS OF "EXCAVATION FOR STRUCTURES BRIDGES B-50-97" SHALL BE THE EXISTING GROUND LINE.

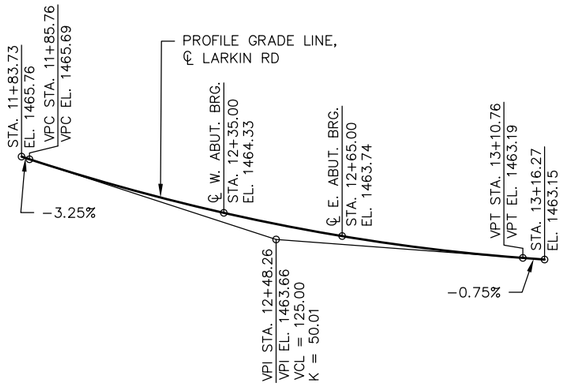
AT ABUTMENTS, CONCRETE POURED UNDER WATER WILL BE ALLOWED AND SHALL BE DONE IN ACCORDANCE WITH SECTION 502.3.5.3 OF THE STANDARD SPECIFICATIONS.

SLAB FALSEWORK SHALL BE SUPPORTED ON PILES OR THE SUBSTRUCTURE, UNLESS AN ALTERNATE METHOD IS APPROVED BY THE ENGINEER.

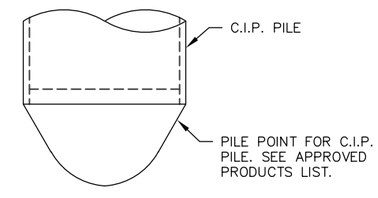
AT THE BACKFACE OF ABUTMENT ALL VOLUME WHICH CANNOT BE PLACED BEFORE ABUTMENT CONSTRUCTION AND IS NOT OCCUPIED BY THE NEW STRUCTURE SHALL BE BACKFILLED WITH STRUCTURE BACKFILL TYPE A.

DO NOT PLACE FILL ABOVE 3'-0" FROM BOTTOM OF ABUTMENT UNTIL SUPERSTRUCTURE IS IN PLACE.

THE EXISTING STRUCTURE (P-50-914) CONSISTS OF THREE 6-FT DIAMETER CORRUGATED STEEL PIPES WITH OVERALL PIPE LENGTHS OF 36-FT AND TOTAL LENGTH ALONG THE ROADWAY OF 22-FT AND IS TO BE REMOVED PER BID ITEM "REMOVING STRUCTURE P-50-914". EXISTING STRUCTURE IS TO BE REMOVED IN STAGES, SEE ROADWAY PLANS FOR DETAILS.



PROFILE GRADE LINE, ℄ LARKIN RD



PILE POINT FOR C.I.P. PILING

PILE POINT SHALL BE INSTALLED ACCORDING TO THE PILE POINT MANUFACTURE'S INSTRUCTIONS. ENSURE PILE POINT WELDS ARE WATERTIGHT.

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-50-97

DRAWN BY JDO | PLANS CK'D ACK

CROSS SECTION, GENERAL NOTES & QUANTITIES

SHEET 2 OF 8

35

8

8

FILE: B-50-0097_02_qty.dwg
PLOT SCALE:

I.D. 8724-04-71

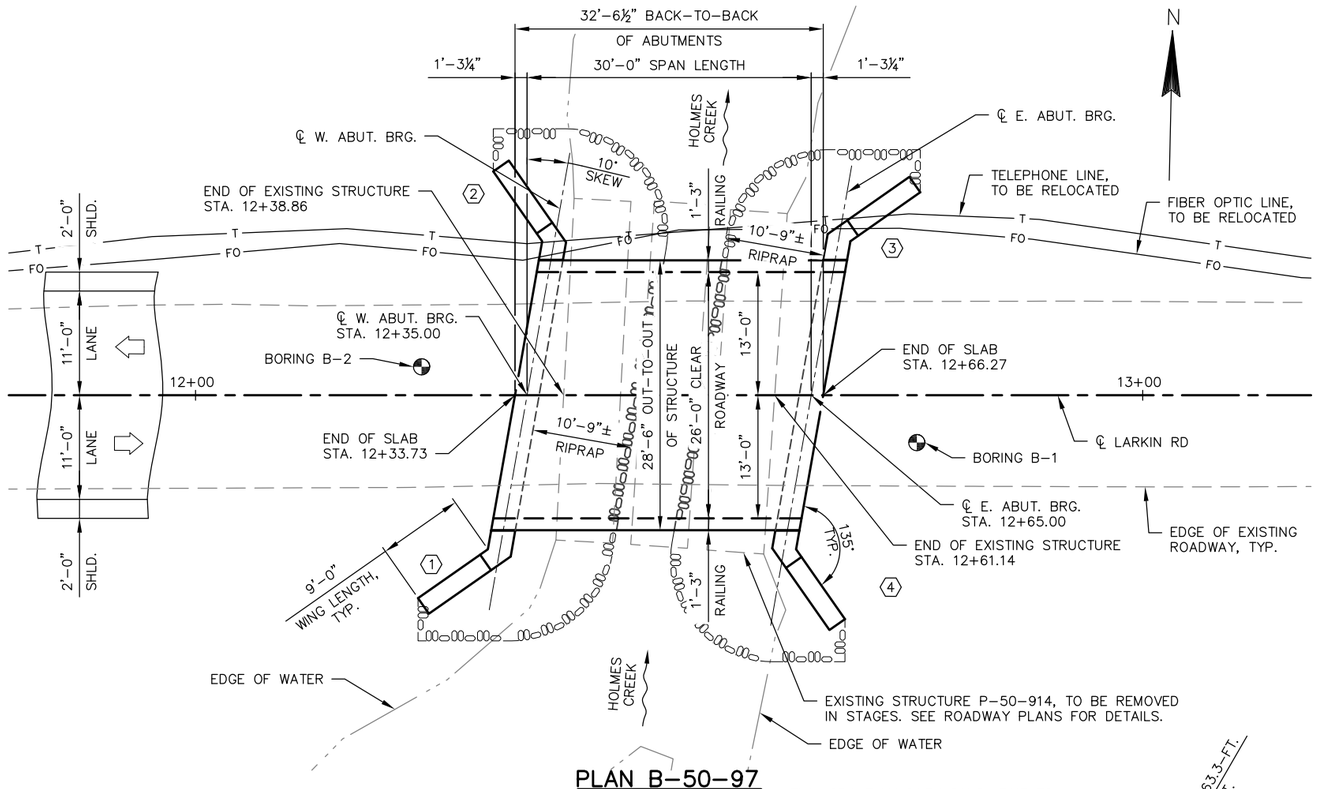
PLOT DATE: Sep 22, 2025

B-50-97 BORINGS

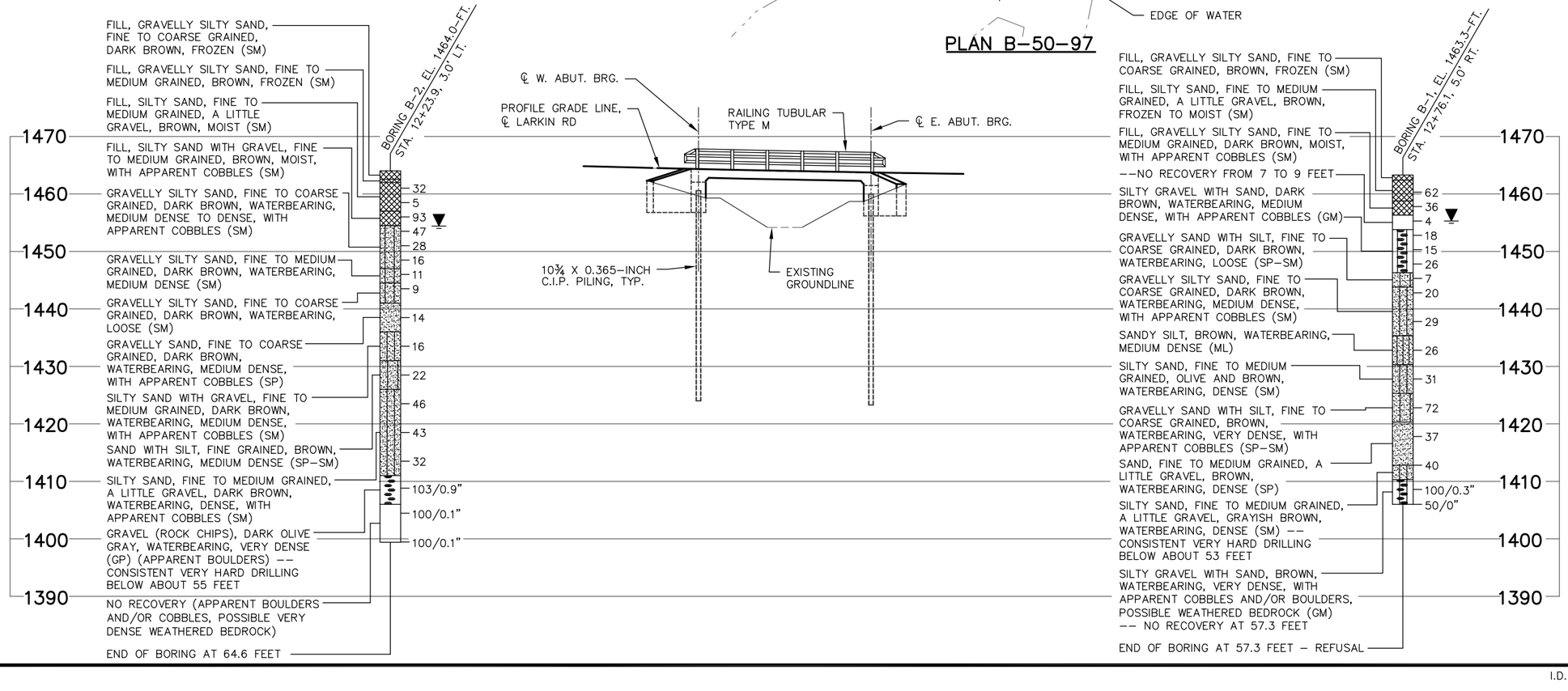
BORING #	DATE COMPLETED	NORTHING (Y)	EASTING (X)
BORING B-1	1/8/2025	331877.5	783457.8
BORING B-2	1/9/2025	331884.0	783405.3
BORINGS COMPLETED BY: AMERICAN ENGINEERING TESTING			
SUBSURFACE INVESTIGATION REPORT: AMERICAN ENGINEERING TESTING			
ALL COORDINATES REFERENCED TO WISCRS, PRICE COUNTY			

NOTE

⬡ INDICATES WING NUMBER



PLAN B-50-97



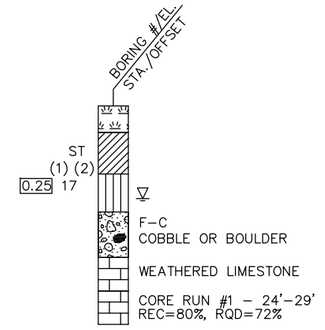
STATE PROJECT NUMBER

8724-04-71

MATERIAL SYMBOLS

ASPHALT	TOPSOIL	PEAT
CONCRETE	FILL	GRAVEL
SAND	CLAY	SILT
BOULDERS OR COBBLES	LIMESTONE	BEDROCK (UNKNOWN)
SHALE	SANDSTONE	IGNEOUS/ META

LEGEND OF BORING



(1) UNCONFINED STRENGTH, AS DETERMINED BY A POCKET PENETROMETER (TSF)

(2) UNLESS OTHERWISE SPECIFIED, THE SPT 'N' VALUE IS BASED ON AASHTO T-206, STANDARD PENETRATION TEST. THE SPT 'N' VALUE PRESENTED HAS NOT BEEN CORRECTED FOR OVERBURDEN PRESSURE OR HAMMER EFFICIENCY.

GROUND WATER ELEVATION

▽ AT TIME OF DRILLING

▼ END OF DRILLING

▼ AFTER DRILLING

ABBREVIATIONS

F-FINE M-MEDIUM C-COARSE ST-SHELBY TUBE

SUBSURFACE EXPLORATION FOR FOUNDATION DESIGN AND BIDDERS INFORMATION

BORINGS WERE COMPLETED AT POINTS APPROXIMATELY AS INDICATED ON THIS DRAWING TO OBTAIN INFORMATION CONCERNING THE CHARACTER OF SUBSURFACE MATERIALS FOUND AT THE SITE. BECAUSE THE INVESTIGATED DEPTHS ARE LIMITED AND THE AREA OF THE BORINGS IS VERY SMALL IN RELATION TO THE ENTIRE SITE, THE WISCONSIN DEPARTMENT OF TRANSPORTATION DOES NOT WARRANT SIMILAR SUBSURFACE CONDITIONS BELOW, BETWEEN, OR BEYOND THESE BORINGS. VARIATIONS IN SOIL CONDITIONS SHOULD BE EXPECTED AND FLUCTUATIONS IN GROUNDWATER LEVELS MAY OCCUR.

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-50-97

DRAWN BY JDO | PLANS CK'D ACK

SUBSURFACE EXPLORATION

SHEET 3 OF 8

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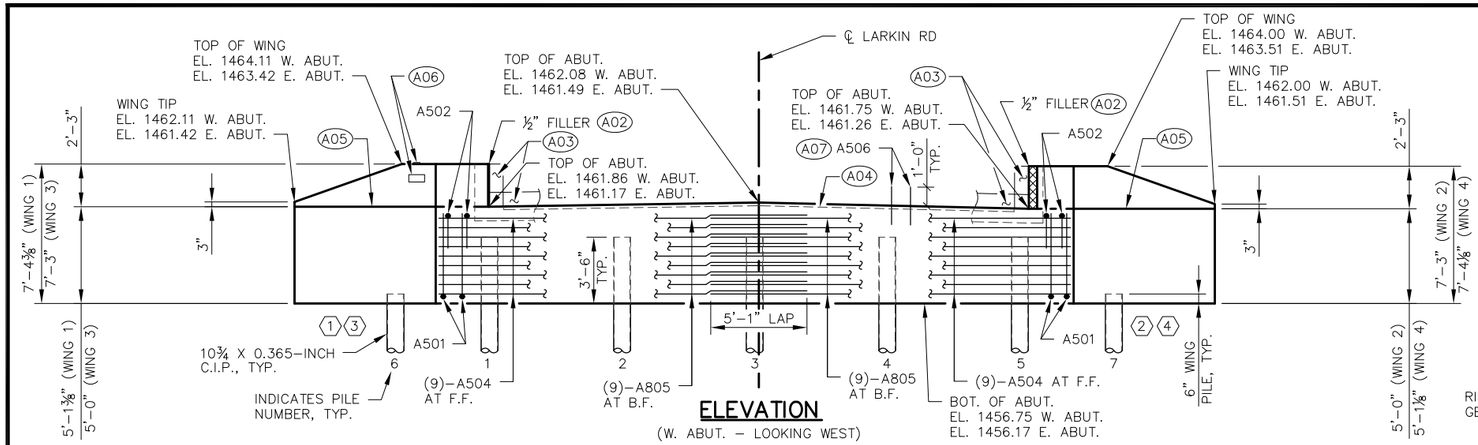
8

I.D. 8724-04-71 PLOT DATE: Aug 18, 2025

FILE: B-50-0097_03_bor.dwg
PLOT SCALE:

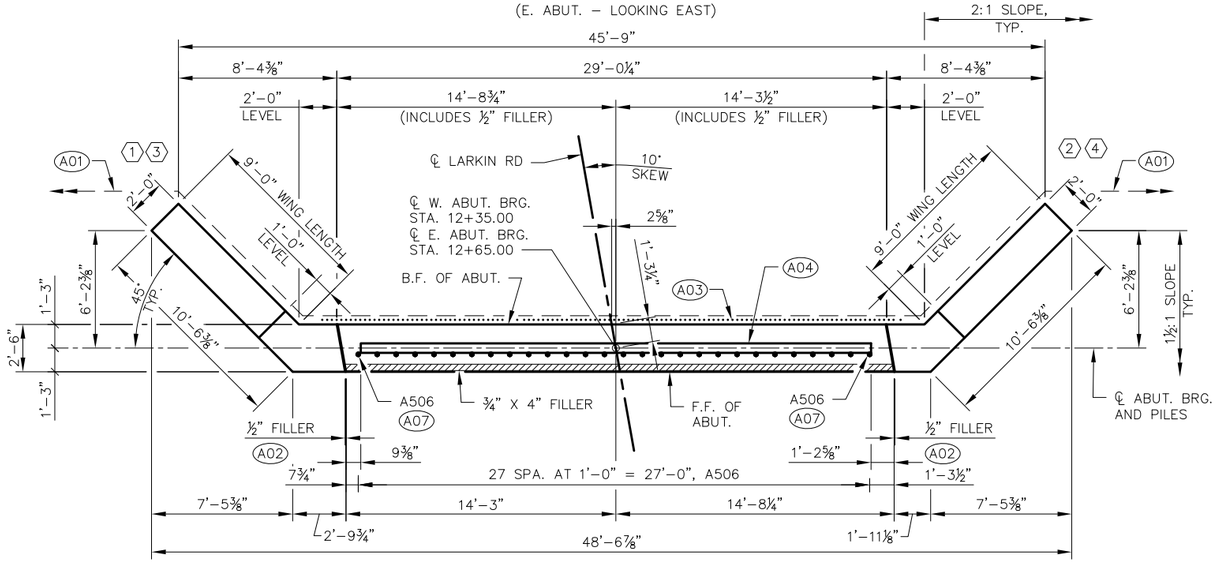
STATE PROJECT NUMBER

8724-04-71

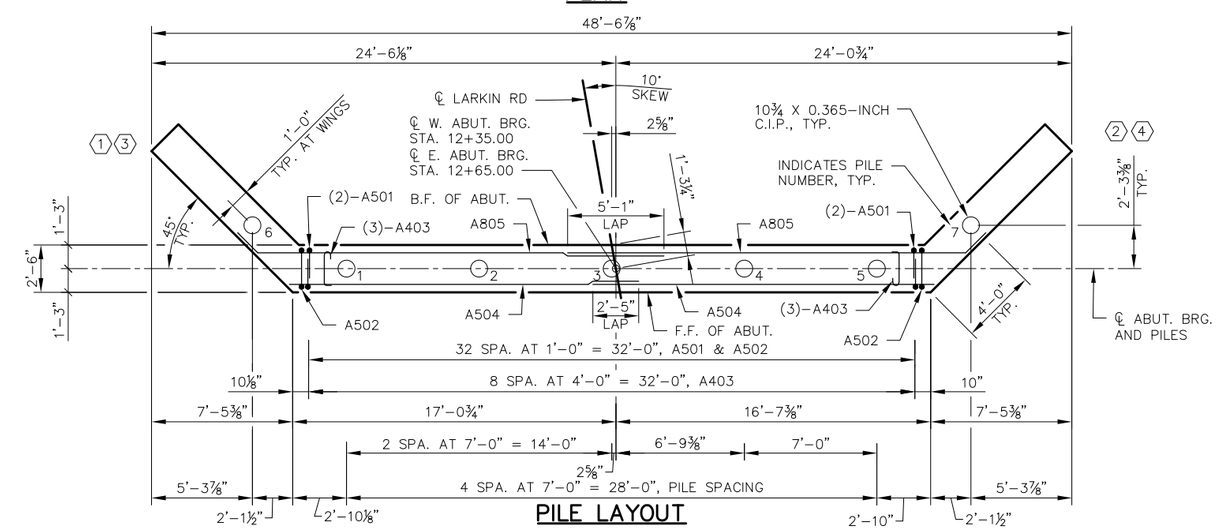


ELEVATION

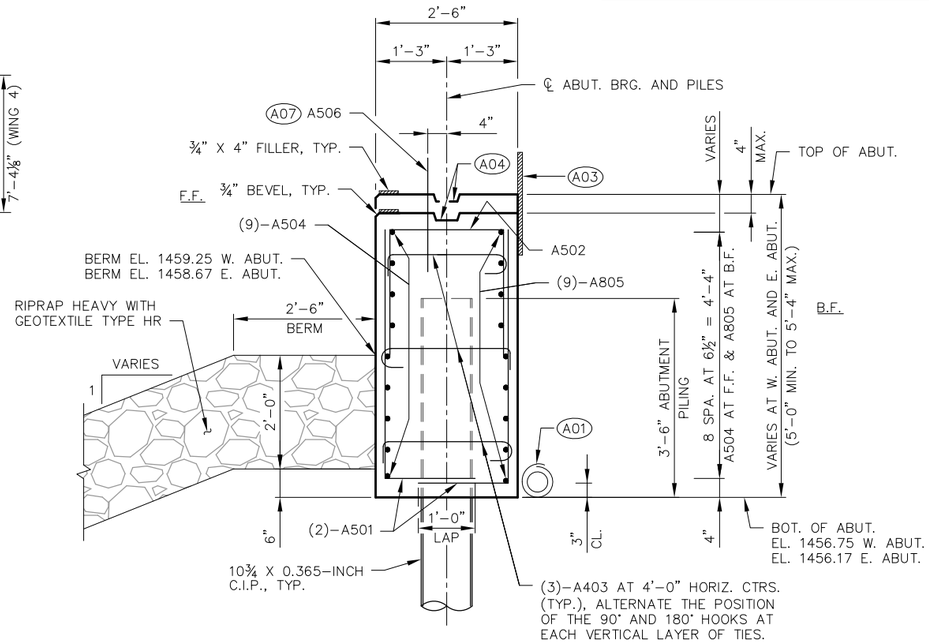
(W. ABUT. – LOOKING WEST)
(E. ABUT. – LOOKING EAST)



PLAN



PILE LAYOUT



TYPICAL SECTION THRU ABUTMENT

NOTES

DO NOT PLACE FILL ABOVE 3'-0" FROM THE BOTTOM OF THE ABUTMENT UNTIL SUPERSTRUCTURE IS IN PLACE.

WEST AND EAST ABUTMENT TO BE SUPPORTED ON 10¾ X 0.365-INCH C.I.P. PILING DRIVEN TO A REQUIRED DRIVING RESISTANCE OF 120 TONS PER PILE AT THE WEST ABUTMENT AND 120 TONS PER PILE AT THE EAST ABUTMENT AS DETERMINED BY THE MODIFIED GATES DYNAMIC FORMULA. ESTIMATED 40 FT PILE LENGTHS AT THE WEST ABUTMENT AND 40 FT PILE LENGTHS AT THE EAST ABUTMENT.

SEE "CROSS SECTION, GENERAL NOTES & QUANTITIES" SHEET FOR C.I.P. PILE SPLICE DETAILS.

(A01) PIPE UNDERDRAIN WRAPPED (6-INCH). SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. ATTACH RODENT SHIELD AT ENDS OF PIPE UNDERDRAIN AS DETAILED ON "ABUTMENT DETAILS" SHEET. RODENT SHIELD SHALL BE INCLUDED WITH THE BID ITEM "PIPE UNDERDRAIN WRAPPED 6-INCH".

(A02) SEAL ALL EXPOSED HORIZONTAL AND VERTICAL SURFACES OF ½" FILLER WITH NON-STAINING GRAY NON-BITUMINOUS JOINT SEALER. (1" DEEP AND HOLD ⅛" BELOW SURFACE OF CONCRETE). ½" FILLER TO EXTEND FROM BRIDGE SEAT TO TOP OF WING.

(A03) 18" RUBBERIZED MEMBRANE WATERPROOFING (R.M.W.), SEAL ALL HORIZONTAL AND VERTICAL JOINTS ON BACK FACE.

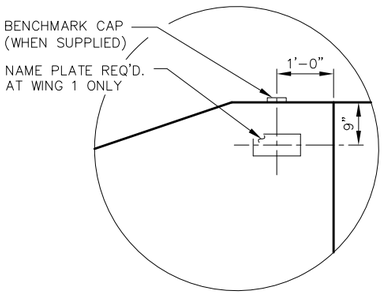
(A04) KEYED CONST. JT. FORMED BY BEVELED 2 X 6

(A05) OPTIONAL KEYED CONST. JT. FORMED BY BEVELED 2 X 6, TYP.

(A06) NAME PLATE & BENCHMARK CAP (WHEN SUPPLIED) AT WING 1 ONLY. SEE "NAME PLATE DETAIL", THIS SHEET.

(A07) A506 BARS MAY BE PLACED AFTER CONCRETE HAS BEEN POURED BUT BEFORE INITIAL SET HAS TAKEN PLACE. EMBED 1'-0" INTO ABUTMENT BODY.

⬡ INDICATES WING NUMBER



NAME PLATE DETAIL

F.F. – FRONT FACE
B.F. – BACK FACE

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-50-97

DRAWN BY JDO | PLANS CK'D ACK

ABUTMENTS

SHEET 4 OF 8

37

8

8

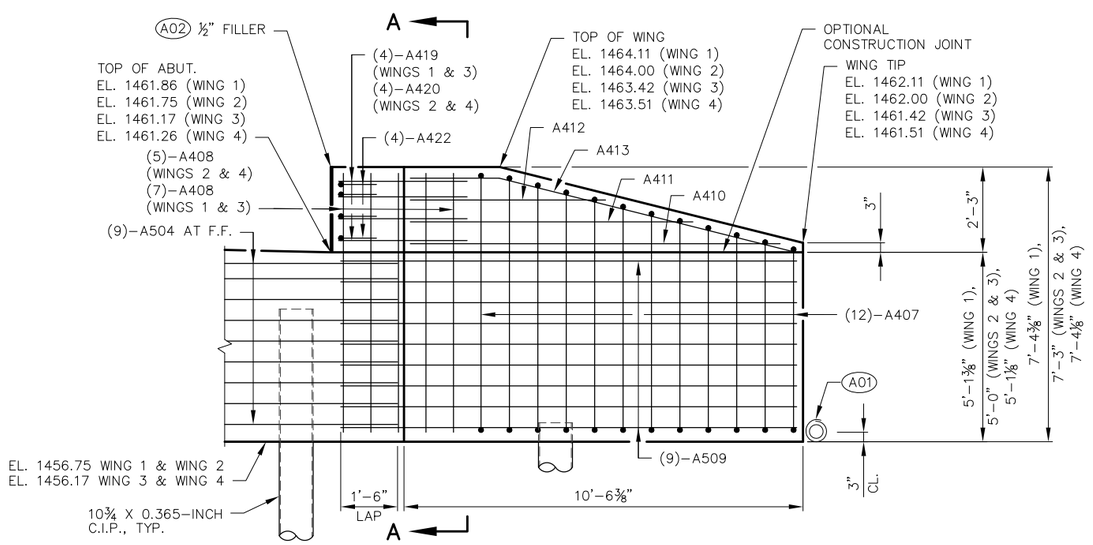
I.D. 8724-04-71

PLOT DATE: Aug 18, 2025

FILE: B500097_04-05_abut.dwg
PLOT SCALE:

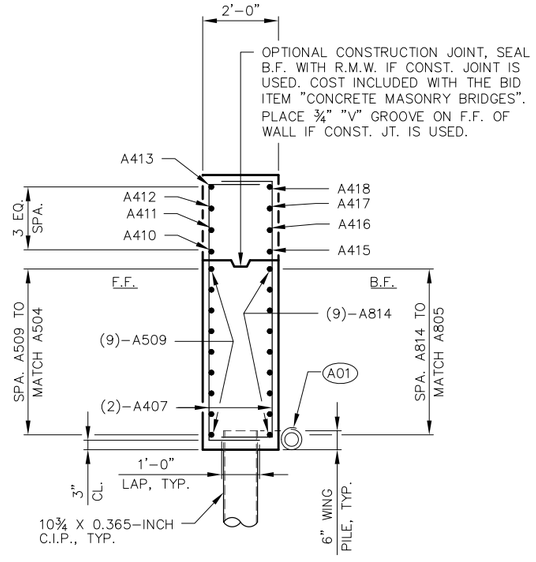
STATE PROJECT NUMBER

8724-04-71

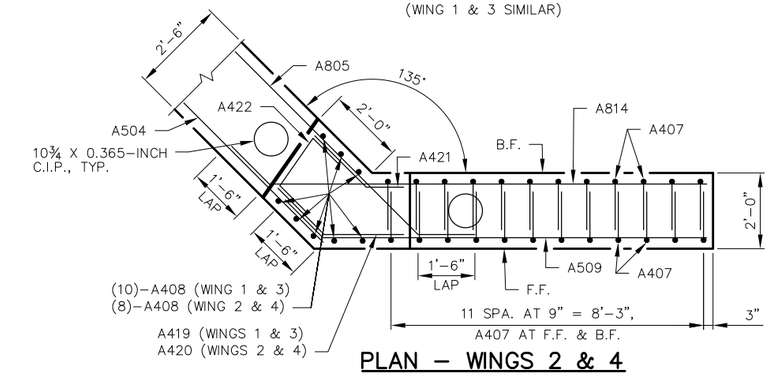


F.F. ELEVATION – WINGS 2 & 4

(WING 1 & 3 SIMILAR)

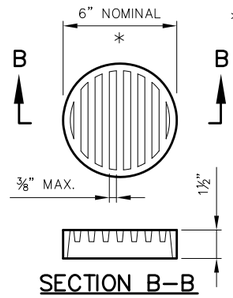


SECTION A–A



PLAN – WINGS 2 & 4

(WING 1 & 3 SIMILAR)



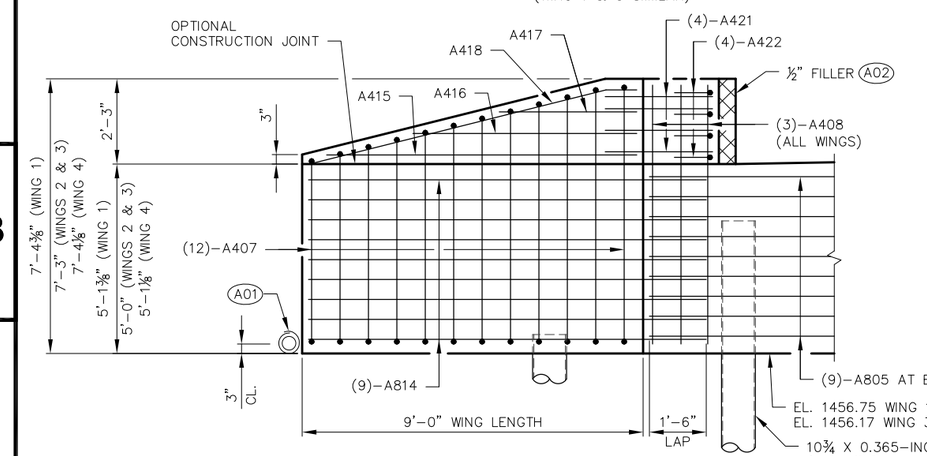
SECTION B–B

* DIMENSIONS ARE APPROXIMATE. THE GRATE IS SIZED TO FIT INTO A PIPE COUPLING. ORIENT SHIELD SO SLOTS ARE VERTICAL.

THE RODENT SHIELD SHALL BE A PVC GRATE SIMILAR TO THIS DETAIL. THE GRATE IS COMMERCIALLY AVAILABLE AS A FLOOR STRAINER. A PIPE COUPLING IS REQUIRED FOR THE ATTACHMENT OF THIS SHIELD TO THE EXPOSED END OF THE PIPE UNDERDRAIN. THE SHIELD SHALL BE FASTENED TO THE PIPE COUPLING WITH TWO OR MORE NO. 10 X 1–INCH STAINLESS STEEL SHEET METAL SCREWS.

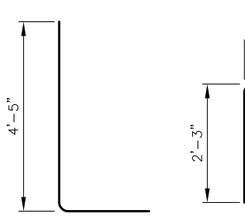
THE RODENT SHIELD. PIPE COUPLING AND SCREWS SHALL BE INCLUDED WITH THE BID ITEM "PIPE UNDERDRAIN WRAPPED 6–INCH".

RODENT SHIELD DETAIL

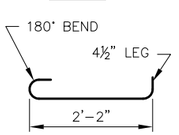


B.F. ELEVATION – WINGS 2 & 4

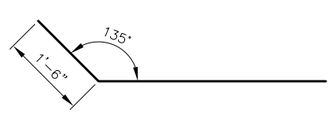
(WING 1 & 3 SIMILAR)



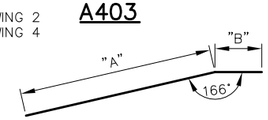
A501 A502 A422



A403



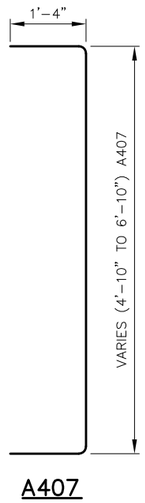
A805, A509 & A814



A413 & A418

BAR BEND DIMENSIONS

MARK	"A"	"B"
A413	8'–1"	2'–4"
A418	8'–1"	0'–9"

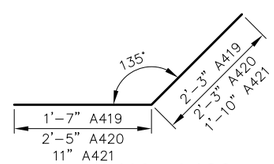


A407

BAR SERIES TABLE

MARK	NO. REQ'D	LENGTH
A407	8 SERIES OF 12	7'–4" TO 9'–4"

BUNDLE AND TAG EACH SERIES SEPARATELY.



A419, A420, & A421

BILL OF BARS BOTH ABUTMENTS

COATED = 2,810 LBS.
UNCOATED = 4,250 LBS.

MARK	COATED	NUMBER W. ABUT.	NUMBER E. ABUT.	LENGTH	BENT	BAR SERIES	LOCATION	
A501		66	66	5'–11"	X		BODY – STIRRUP – F.F. & B.F.	VERT.
A502		33	33	6'–5"	X		BODY – STIRRUP – TOP	VERT.
A403		27	27	3'–1"	X		BODY – TIES	HORIZ.
A504		18	18	18'–1"			BODY – F.F.	HORIZ.
A805		18	18	22'–11"	X		BODY – B.F.	HORIZ.
A506	X	28	28	2'–0"			BODY – TOP DOWELS	VERT.
A407	X	48	48	8'–4"	X	⚠	WINGS 1 THRU 4 – STIRRUP – F.F. & B.F.	VERT.
A408	X	18	18	6'–10"			WINGS 1 THRU 4 – F.F. & B.F.	VERT.
A509	X	18	18	11'–9"	X		WINGS 1 THRU 4 – F.F.	HORIZ.
A410	X	2	2	9'–9"			WINGS 1 THRU 4 – F.F.	HORIZ.
A411	X	2	2	7'–5"			WINGS 1 THRU 4 – F.F.	HORIZ.
A412	X	2	2	5'–1"			WINGS 1 THRU 4 – F.F.	HORIZ.
A413	X	2	2	10'–5"	X		WINGS 1 THRU 4 – F.F. – TOP	HORIZ.
A814	X	18	18	13'–3"	X		WINGS 1 THRU 4 – B.F.	HORIZ.
A415	X	2	2	8'–2"			WINGS 1 THRU 4 – B.F.	HORIZ.
A416	X	2	2	5'–10"			WINGS 1 THRU 4 – B.F.	HORIZ.
A417	X	2	2	3'–7"			WINGS 1 THRU 4 – B.F.	HORIZ.
A418	X	2	2	8'–10"	X		WINGS 1 THRU 4 – B.F. – TOP	HORIZ.
A419	X	4	4	3'–9"	X		WINGS 1 AND 3 – F.F. CORNER	HORIZ.
A420	X	4	4	4'–7"	X		WINGS 2 AND 4 – F.F. CORNER	HORIZ.
A421	X	8	8	2'–8"	X		WINGS 1 THRU 4 – B.F. CORNER	HORIZ.
A422	X	8	8	4'–0"	X		WINGS 1 THRU 4 – TOP CORNER	HORIZ.

THE FIRST DIGIT OF A BAR MARK SIGNIFIES THE BAR SIZE.

ALL BAR BEND DIMENSIONS ARE OUT TO OUT OF BAR.

BUNDLE EACH ABUTMENT BARS SEPARATELY.

⚠ LENGTH SHOWN FOR BAR IS AN AVERAGE LENGTH AND SHOULD ONLY BE USED FOR BAR WEIGHT CALCULATIONS. SEE "BAR SERIES TABLE" FOR ACTUAL LENGTHS.

NOTES

DO NOT PLACE FILL ABOVE 3'–0" FROM THE BOTTOM OF THE ABUTMENT UNTIL SUPERSTRUCTURE IS IN PLACE.

WEST AND EAST ABUTMENT TO BE SUPPORTED ON 10¾ X 0.365–INCH C.I.P. PILING DRIVEN TO A REQUIRED DRIVING RESISTANCE OF 120 TONS PER PILE AT THE WEST ABUTMENT AND 120 TONS PER PILE AT THE EAST ABUTMENT AS DETERMINED BY THE MODIFIED GATES DYNAMIC FORMULA. ESTIMATED 40 FT PILE LENGTHS AT THE WEST ABUTMENT AND 40 FT PILE LENGTHS AT THE EAST ABUTMENT.

SEE "CROSS SECTION, GENERAL NOTES & QUANTITIES" SHEET FOR C.I.P. PILE SPLICE DETAILS.

(A01) PIPE UNDERDRAIN WRAPPED (6–INCH). SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. ATTACH RODENT SHIELD AT ENDS OF PIPE UNDERDRAIN AS DETAILED ON THIS SHEET. RODENT SHIELD SHALL BE INCLUDED WITH THE BID ITEM "PIPE UNDERDRAIN WRAPPED 6–INCH".

(A02) SEAL ALL EXPOSED HORIZONTAL AND VERTICAL SURFACES OF ½" FILLER WITH NON–STAINING GRAY NON–BITUMINOUS JOINT SEALER. (1" DEEP AND HOLD ⅛" BELOW SURFACE OF CONCRETE.) ½" FILLER TO EXTEND FROM BRIDGE SEAT TO TOP OF WING.

F.F. – FRONT FACE
B.F. – BACK FACE

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B–50–97

DRAWN BY JDO | PLANS CK'D ACK

ABUTMENT DETAILS

SHEET 5 OF 8

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FILE: B500097_04-05_abut.dwg
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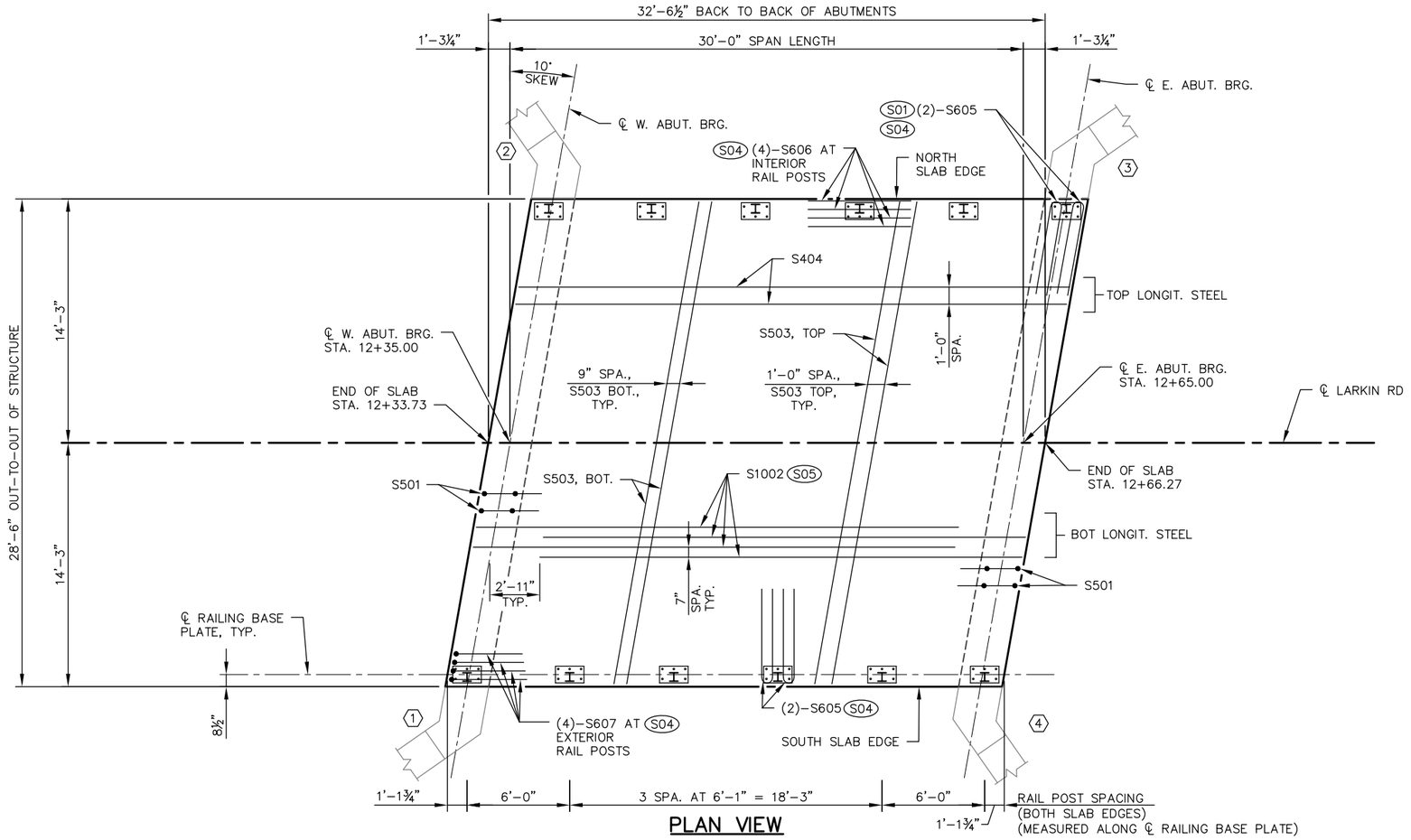
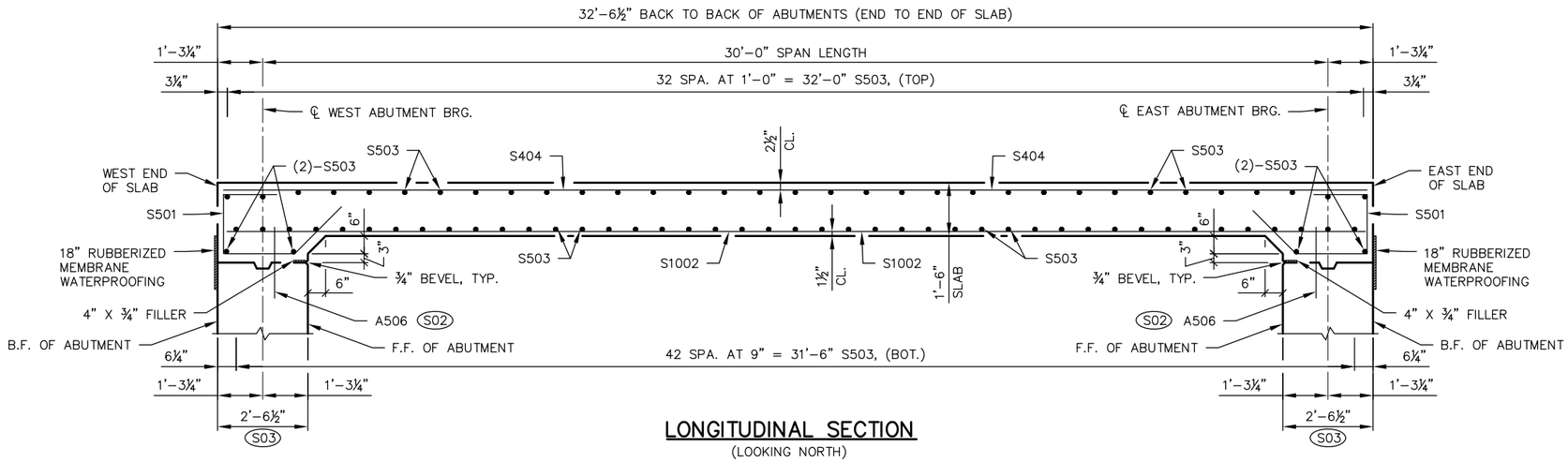


PLAN VIEW



LONGITUDINAL SECTION

(LOOKING NORTH)

NOTES

TOP TRANSVERSE BARS IN SLAB SHALL BE SUPPORTED BY INDIVIDUAL BAR CHAIRS AT APPROXIMATELY 3'-0" CENTERS EACH WAY.

BOTTOM LONGITUDINAL BARS SHALL BE SUPPORTED BY CONTINUOUS BAR CHAIRS AT APPROXIMATELY 4'-0" CENTERS.

ALL SLAB THICKNESS DIMENSIONS ARE MINIMUM. ANY TOLERANCES NECESSARY TO CORRECT CONSTRUCTION DISCREPANCIES ARE TO BE PLUS (+).

RAILING TO BE INSTALLED ON THE SLAB AFTER FALSEWORK HAS BEEN RELEASED.

(S01) ADJUST ORIENTATION OF S605 BAR AT END POST NEAR WINGS 1 & 3 TO ENSURE CLEAR COVER AT END OF SLAB.

(S02) SEE "ABUTMENTS" SHEET FOR PLACEMENT OF A506 BARS

(S03) DIMENSION IS TAKEN PARALLEL TO ℄ LARKIN RD

(S04) SEE "RAILING TUBULAR TYPE M" SHEET FOR PLACEMENT OF RAIL POST REINFORCEMENT.

(S05) EXTEND ONE END OF THE S1002 BAR TO 2" CLEAR OF ONE BACK FACE OF ABUTMENT. ALTERNATE BETWEEN WEST AND EAST ABUTMENTS ACROSS ENTIRE SLAB.

⬡ INDICATES WING NUMBER

F.F. – FRONT FACE
B.F. – BACK FACE

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STATE OF WISCONSIN
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STRUCTURE B-50-97

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SUPERSTRUCTURE

SHEET 6 OF 8

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I.D. 8724-04-71

PLOT DATE: Aug 18, 2025

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PLOT SCALE:

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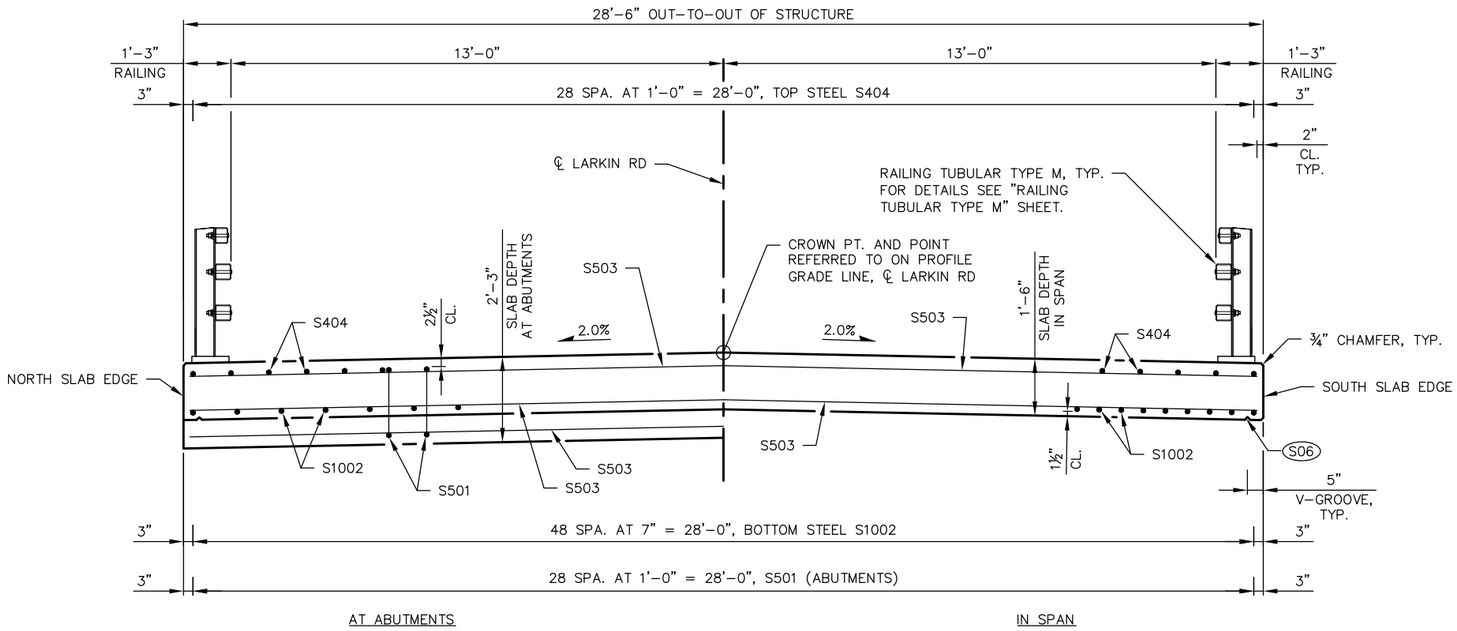


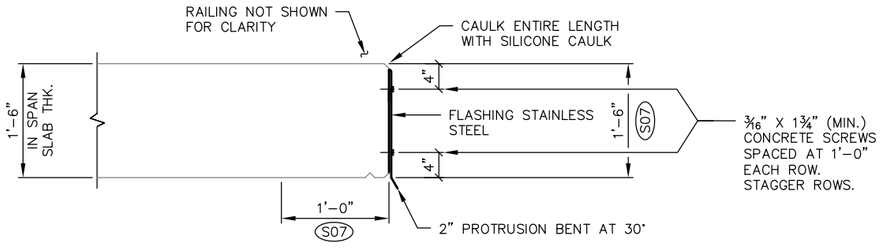
CROSS SECTION THRU ROADWAY

(LOOKING EAST)

SURVEY TOP OF SLAB ELEVATIONS

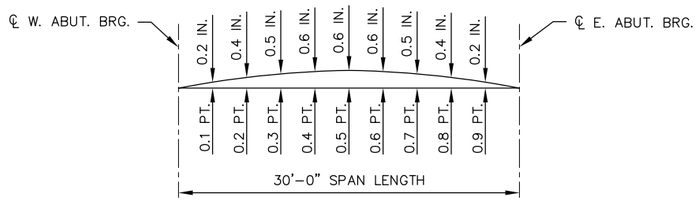
	℄ W. ABUT. BRG.	5/10 PT.	℄ E. ABUT. BRG.
NORTH SLAB EDGE			
℄ LARKIN RD			
SOUTH SLAB EDGE			

PRIOR TO RELEASING SLAB FALSEWORK, TAKE TOP OF SLAB ELEVATIONS AT THE ℄ OF ABUTMENTS AND AT 5/10 POINTS TO VERIFY CAMBER. TAKE ELEVATIONS ALONG EDGE OF SLAB AND REFERENCE LINE. RECORD THE ELEVATIONS IN THE ABOVE TABLE FOR THE "AS BUILT" PLANS.



FLASHING DETAIL

(INSTALL FLASHING ON BOTH THE NORTH & SOUTH SLAB EDGES)



SLAB CAMBER DIAGRAM

TO DETERMINE FALSEWORK ELEVATION AT EDGE OF SLAB, CROWN OR REFERENCE LINE FOLLOW THIS PROCEDURE:

	TOP OF SLAB ELEVATION AT FINAL GRADE
LESS	SLAB THICKNESS
PLUS	CAMBER
PLUS	FORM SETTLEMENT/DEFLECTION DUE TO PLACEMENT OF SLAB CONCRETE (TO BE COMPUTED BY THE CONTRACTOR)
EQUALS	TOP OF SLAB FALSEWORK ELEVATION.

TOP OF SLAB ELEVATIONS

SPAN PT	NORTH SLAB EDGE	℄ LARKIN RD	SOUTH SLAB EDGE
℄ W. ABUT.	1464.00	1464.33	1464.11
0.1	1463.93	1464.27	1464.04
0.2	1463.87	1464.20	1463.97
0.3	1463.81	1464.14	1463.91
0.4	1463.75	1464.08	1463.85
0.5	1463.69	1464.02	1463.79
0.6	1463.63	1463.96	1463.73
0.7	1463.58	1463.90	1463.67
0.8	1463.52	1463.85	1463.61
0.9	1463.47	1463.79	1463.56
℄ E. ABUT.	1463.42	1463.74	1463.51

BILL OF BARS
SUPERSTRUCTURE — COATED = 10,210 LBS.

MARK	NUMBER COATED	NUMBER UNCOATED	LENGTH	BENT	BAR SERIES	LOCATION	
S501	58		7'-0"	X		SLAB AT ABUTMENT – TIES	LONGIT.
S1002	49		28'-3"			SLAB – BOTTOM	LONGIT.
S503	80		28'-7"			SLAB – TOP, BOT. & OVER ABUTMENTS	TRANS.
S404	29		32'-2"			SLAB – TOP	LONGIT.
S605	24		11'-6"	X		SLAB – TOP AT RAIL POSTS	TRANS.
S606	32		6'-0"			SLAB – TOP AT INTERIOR RAIL POSTS	LONGIT.
S607	16		4'-8"	X		SLAB – TOP AT EXTERIOR RAIL POSTS	LONGIT.

THE FIRST OR FIRST TWO DIGITS OF A FOUR DIGIT BAR MARK SIGNIFIES THE BAR SIZE.

ALL BAR BEND DIMENSIONS ARE OUT TO OUT OF BAR.

NOTES

CAMBER SPAN AS SHOWN TO PROVIDE FOR DEAD LOAD DEFLECTION AND FUTURE CREEP. CAMBER DOES NOT INCLUDE ALLOWANCE FOR FORM SETTLEMENT.

THE BID ITEM "FLASHING STAINLESS STEEL" SHALL INCLUDE PROVIDING AND INSTALLING THE STAINLESS STEEL FLASHING, SILICONE CAULK, CONCRETE SCREWS, AND CLEANING THE EDGE OF THE DECK PRIOR TO ATTACHMENT OF THE FLASHING.

FLASHING TO BE INSTALLED AFTER PROTECTIVE SURFACE TREATMENT APPLICATION.

CONCRETE SCREWS SHALL BE 410 STAINLESS STEEL.

EXTEND FLASHING TO F.F. OF ABUTMENT.

TOP OF FLASHING TO BEGIN APPROX. 1–INCH BELOW TOP OF SLAB SURFACE.

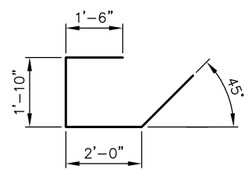
THE FLASHING IS TO BE A CONSTANT HEIGHT BASED ON THE THINNEST SLAB DEPTH OVER THE BRIDGE LENGTH.

PROVIDE 2" MINIMUM FLASHING OVERLAP, FASTEN WITH 3/16" X 2" (MIN.) CONCRETE SCREWS.

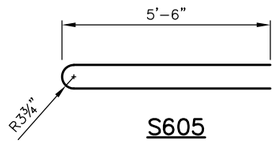
CAULK SHALL BE NON–STAINING, GRAY NON–BITUMINOUS JOINT SEALER.

(S06) ¾" V–GROOVE. EXTEND V–GROOVE TO 6" FROM FRONT FACE OF ABUTMENT BODY. V–GROOVES ARE REQUIRED.

(S07) COAT WITH "PROTECTIVE SURFACE TREATMENT" AS PER THE STANDARD SPECIFICATIONS. PROTECTIVE SURFACE TREATMENT TO BE APPLIED TO THE EDGE OF SLAB AND 1'–0" UNDER SLAB BETWEEN F.F. OF ABUTMENTS.



S501



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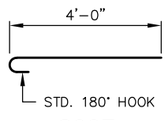

S607

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B–50–97

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SUPERSTRUCTURE DETAILS

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I.D. 8724–04–71 PLOT DATE: Aug 18, 2025

FILE: B500097_06_07_super.dwg
PLOT SCALE:

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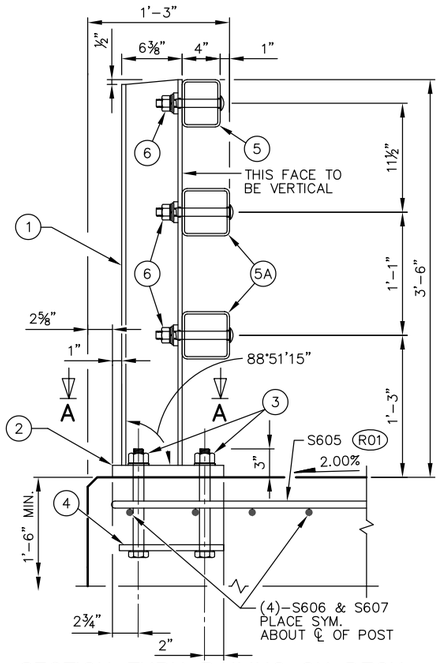
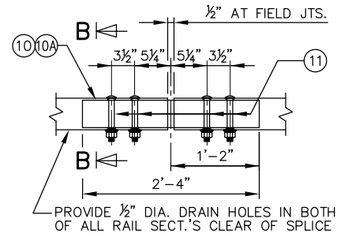
STATE PROJECT NUMBER

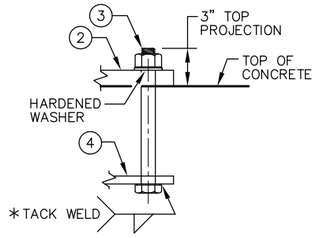
8724-04-71



SECTION THRU RAILING ON DECK

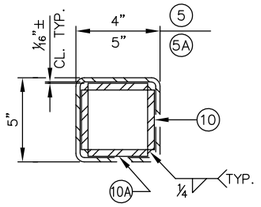


FIELD ERECTION JOINT DETAIL

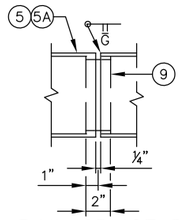


ANCHOR BOLTS

∗ANCHOR BOLT ASSEMBLY MAY BE TACK WELDED, EITHER IN THE SHOP, OR IN THE FIELD AFTER THE ANCHOR PLATE IS PLACED.



SECTION B-B



SHOP RAIL SPLICE DETAIL

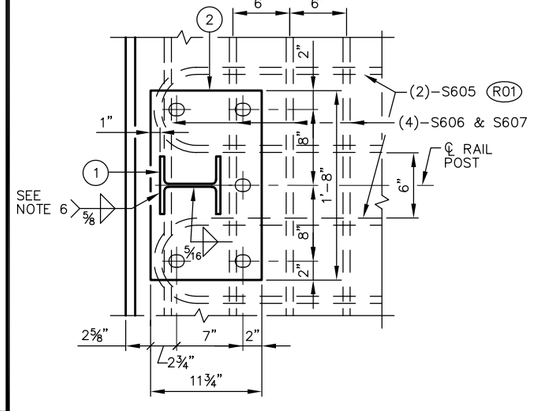
(LOCATION MUST BE SHOWN ON SHOP DRAWINGS)

LEGEND

1. W6 x 25 WITH 1⅛" x 1½" HORIZ. SLOTS ON EACH SIDE OF POST FOR BOLT NO. 6. CUT BOTTOM OF POST TO MATCH CROSS SLOPE OF ROADWAY. PLACE POST VERTICAL. PLACE POSTS NORMAL TO GRADE LINE.
2. PLATE 1¼" x 11¾" x 1'-8" WITH 1 7/16" DIA. OVERSIZED HOLES FOR ANCHOR BOLTS NO. 3. WELD TO NO. 1 AS SHOWN.
3. ASTM A449 – 1⅛" DIA. ANCHOR BOLTS WITH NUT AND HARDENED WASHER (ALL GALVANIZED). 5 REQ'D. PER POST. THREAD 3" AND PLACE NORMAL TO PLATE NO. 2. CHAMFER TOP OF BOLTS BEFORE THREADING. USE 1'-9" LONG IN ABUTMENT WINGS. AT POSTS ON CONCRETE SLAB SUPERSTRUCTURES WHERE THE SLAB THICKNESS IS > 16" USE 1'-3" LONG. USE 10¾" LONG AT ALL OTHER LOCATIONS. (AN EQUIVALENT THREADED ROD WITH NUTS AND HARDENED WASHERS MAY BE SUBSTITUTED FOR ANCHOR BOLTS IN WINGS IF REQ'D. FOR CONSTRUCTABILITY.)
4. ⅝" x 11" x 1'-8" ANCHOR PLATE (GALVANIZED) WITH 1 3/16" DIA. HOLES FOR ANCHOR BOLTS NO. 3
5. TS 5 x 4 x 0.25 STRUCTURAL TUBING. ATTACH TO NO. 1 WITH NO. 6.
5A. TS 5 x 5 x 0.25 STRUCTURAL TUBING. ATTACH TO NO. 1 WITH NO. 6.
6. ⅞" DIA. A325 SLOTTED ROUND HEAD BOLT WITH NUT, 3/16" x 1⅝" x 1⅝" MIN. WASHER, AND LOCK WASHER (2 REQ'D. AT EACH RAIL TO POST LOCATION.)
9. SPLICE SLEEVE FABRICATED FROM ¼" PLATE. PROVIDE "SLIDING FIT".
10. ⅜" x 3⅝" x 2'-4" PLATE. 2 PER RAIL. USED IN NO. 5 & 5A.
10A. ⅜" x 2⅝" x 2'-4" PLATE USED IN NO. 5, ⅜" x 3⅝" x 2'-4" PLATE USED IN NO. 5A. 2 PER RAIL.
11. ⅞" DIA. A325 ROUND HEAD BOLT WITH NUT, WASHER AND LOCK WASHER. USE 15/16" x 1¼" LONGIT. SLOTTED HOLES IN PLATE NO. 10A. AT FIELD JOINTS AND 15/16" x 2¼" MIN. LONGIT. SLOTTED HOLES AT EXP. JOINTS IN PLATE NO. 10A. PROVIDE 15/16" DIA. ROUND HOLES IN TUBES NO. 5 AND NO. 5A.

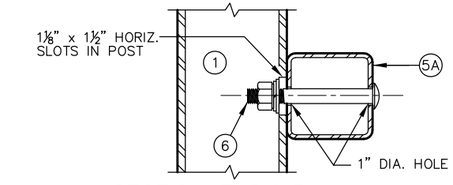
NOTES

1. BID ITEM SHALL BE "RAILING TUBULAR TYPE M" WHICH INCLUDES ALL ITEMS SHOWN.
2. RAIL POST AND BASE PLATES SHALL CONFORM TO THE REQUIREMENTS OF ASTM A709 GRADE 50. HOLLOW RAILING STRUCTURAL TUBING SHALL CONFORM TO THE REQUIREMENTS OF ASTM A500 GRADE B OR C WITH A CERTIFIED FY = 50 KSI. ANCHOR PLATES, AND SPLICE TUBE PLATES SHALL CONFORM TO THE REQUIREMENTS OF ASTM A709 GRADE 36.
3. THE NUT SECURING THE POST BASE PLATE TO THE CONCRETE SHALL BE TIGHTENED TO A SNUG FIT AND GIVEN AN ADDITIONAL 1/8 TURN.
4. RAILS SHALL BE CONTINUOUS OVER A MINIMUM OF THREE (3) POSTS WITHOUT SPLICES WHERE POSSIBLE.
5. ENDS OF TUBE SECTIONS SHALL BE SAWED. GRIND SMOOTH EXPOSED EDGES. ALL CUT ENDS SHALL BE TRUE AND SMOOTH.
6. WELD IS THE SAME ON BOTH FLANGES. FLANGE WELD DOES NOT REQUIRE MAGNETIC PARTICLE TESTING.
7. FILL BOLT SLOT OPENINGS IN POST SHIMS AND PLATE NO. 2 AND CAULK AROUND PERIMETER OF PLATE NO. 2 WITH NON-STAINING GRAY NON-BITUMINOUS JOINT SEALER. STEEL POST SHIMS MAY BE USED UNDER POSTS WHERE REQ'D. FOR ALIGNMENT.
8. POST BASE PLATES SHALL BE FLAT WITH ALL SURFACES SMOOTH AND FREE FROM WARP AND ALL EDGES SMOOTH, STRAIGHT AND VERTICAL. ALL PLATE CUTS SHALL BE MACHINE OR MACHINE FLAME CUT.
9. ALL MATERIAL SHALL BE GALVANIZED AFTER FABRICATION. PRIOR TO GALVANIZING, ALL STEEL RAILING POSTS & STEEL TUBING SHALL BE GIVEN A NO. 6 BLAST CLEANING BY S.S.P.C. SPECIFICATIONS.



SECTION A-A

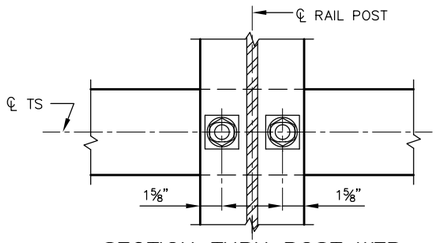
R01 TIE TO TOP MAT OF STEEL.



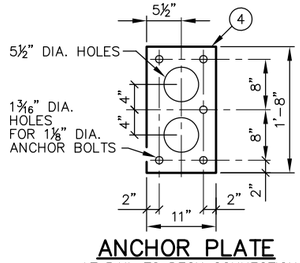
SECTION THRU RAIL

NOTE: CONNECTIONS AT LOWER RAILS SHOWN. CONNECTIONS AT TOP RAIL SIMILAR.

TYPICAL RAIL TO POST CONNECTIONS

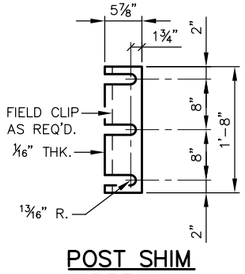


SECTION THRU POST WEB

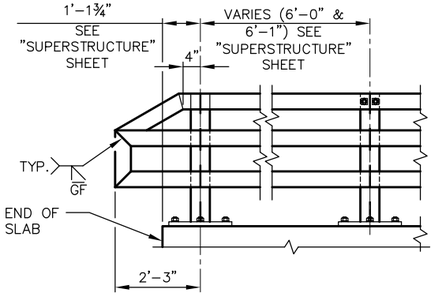


ANCHOR PLATE

AT RAIL TO DECK CONNECTION



POST SHIM DETAIL



PART ELEVATION OF RAILING

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-50-97

DRAWN BY JDO | PLANS CK'D ACK

RAILING TUBULAR TYPE M

SHEET 8 OF 8

41

I.D. 8724-04-71

PLOT DATE: Aug 18, 2025

FILE: B500097_08_mrail.dwg
PLOT SCALE:

8

8

EST APPROAC

STATION	DISTANCE	AREA (SF)			INCREMENTAL VOL (CY) (UNADJUSTED)			CUMULATIVE VOL (CY)		
		CUT	SALVAGED/UNUSABLE PAVEMENT MATERIAL	FILL	CUT	SALVAGED/UNUSABLE PAVEMENT MATERIAL	FILL	CUT 1.00	EXPANDED FILL 1.25	MASS ORDINATE
					NOTE 1	NOTE 2	NOTE 3	NOTE 1		NOTE 4
113.3	0.00	25.2	0.00	1.1	0	0	0	0	0	0
1201.00	1.2	25.05	0.00	11.4	1	0	9	1	11	5
120.3	.3	23.	0.00	1.3	7	0	4	23	1	7
1223.4	14.	22.02	0.00	52.0	12	0	20	35	41	-6
1231.44	.5	21.1	0.00	3.34	6	0	13	41	5	1
1233.3	2.2	11.55	0.00	14.4	1	0	2	42	0	1
STRUCTURE B5000										
			EST APPROAC TOTALS		42	0	4			

EAST APPROACH

STATION	DISTANCE	AREA (SF)			INCREMENTAL VOL (CY) (UNADJUSTED)			CUMULATIVE VOL (CY)		
		CUT	SALVAGED/UNUSABLE PAVEMENT MATERIAL	FILL	CUT	SALVAGED/UNUSABLE PAVEMENT MATERIAL	FILL	CUT 1.00	EXPANDED FILL 1.25	MASS ORDINATE
					NOTE 1	NOTE 2	NOTE 3	NOTE 1		NOTE 4
STRUCTURE B-50-0097										
12+66.27	0.00	10.03	0.00	17.47	0	0	0	0	0	0
12+68.56	2.29	21.58	0.00	32.86	1	0	2	1	3	-2
12+76.51	7.95	22.66	0.00	39.27	.	0	11	8	1	-8
121.2	14.	23.5	0.00	31.55	13	0	1	21	40	1
131.2	25.00	23.	0.00	1.00	22	0	23	43	69	2
			EAST APPROAC TOTALS		43	0	55			
			PROJECT TOTALS		5	0	103			

NOTES	
1 CUT	CUT INCLUDES SALVAGED/UNUSABLE PAVEMENT MATERIAL
2 SALVAGED/UNUSABLE PAVEMENT MATERIAL	TIS DOES NOT SO UP IN CROSS SECTIONS.
3 FILL	DOES NOT INCLUDE UNUSABLE PAVEMENT EXC VOLUME
4 MASS ORDINATE	(CUT)(FILLFILL FACTOR)(SALVAGED/UNUSABLE PAVEMENT MATERIAL)

9

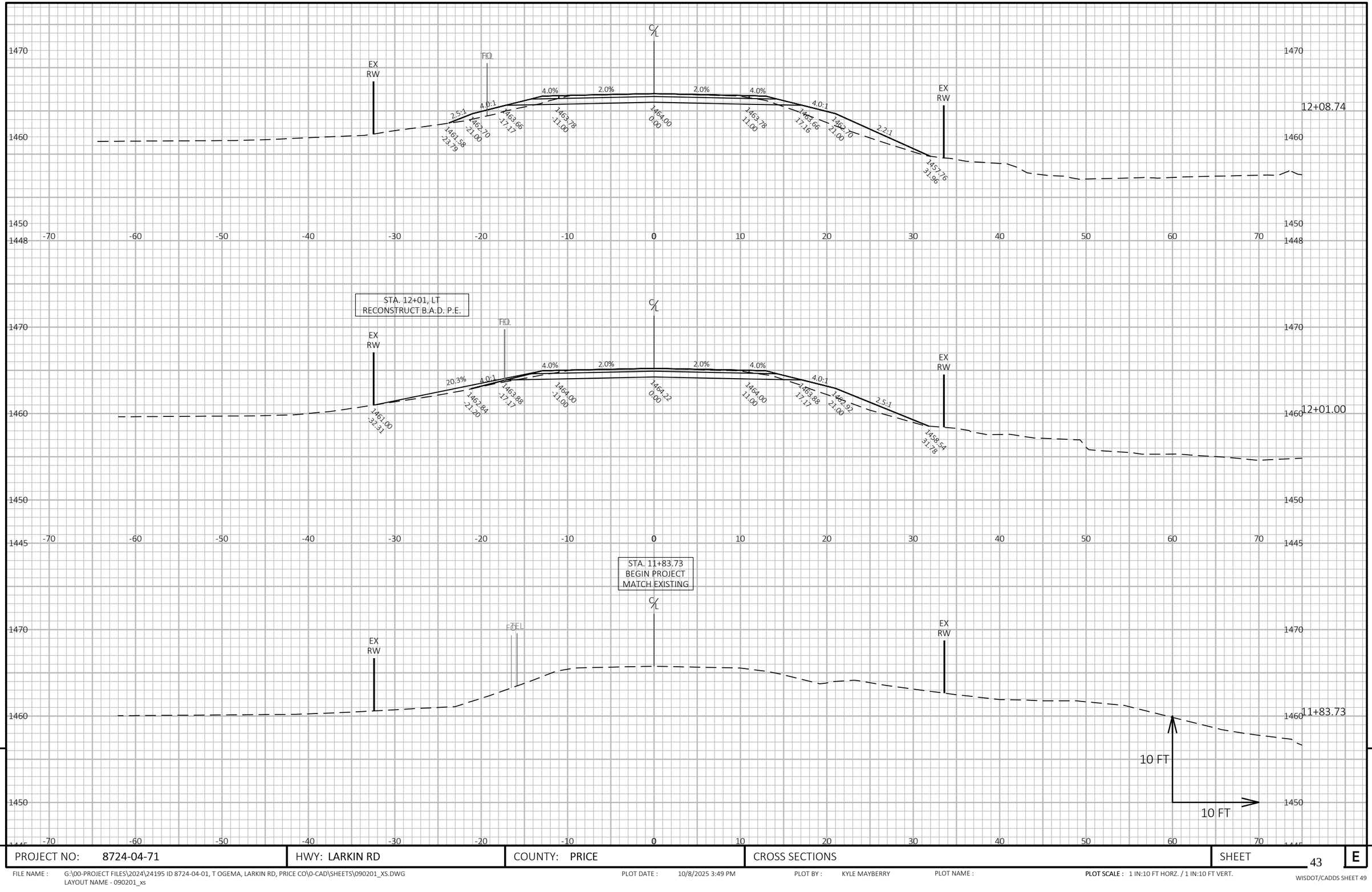
9

PROJECT NO 24041 | Y LARIN RD | COUNTY PRICE | EARTOR DATA | SEET 42 | E

FILE NAME G00PROJECT FILES20242415 ID 240401 T OGEMA LARIN RD PRICE C00CADSEETS00101E.DG
LAYOUT NAME 00101

PLOT DATE 10/10/2025 02 AM PLOT BY YLE MAYBERRY PLOT NAME PLOT SCALE 1 1

ISDOT/CADDS SEET 4



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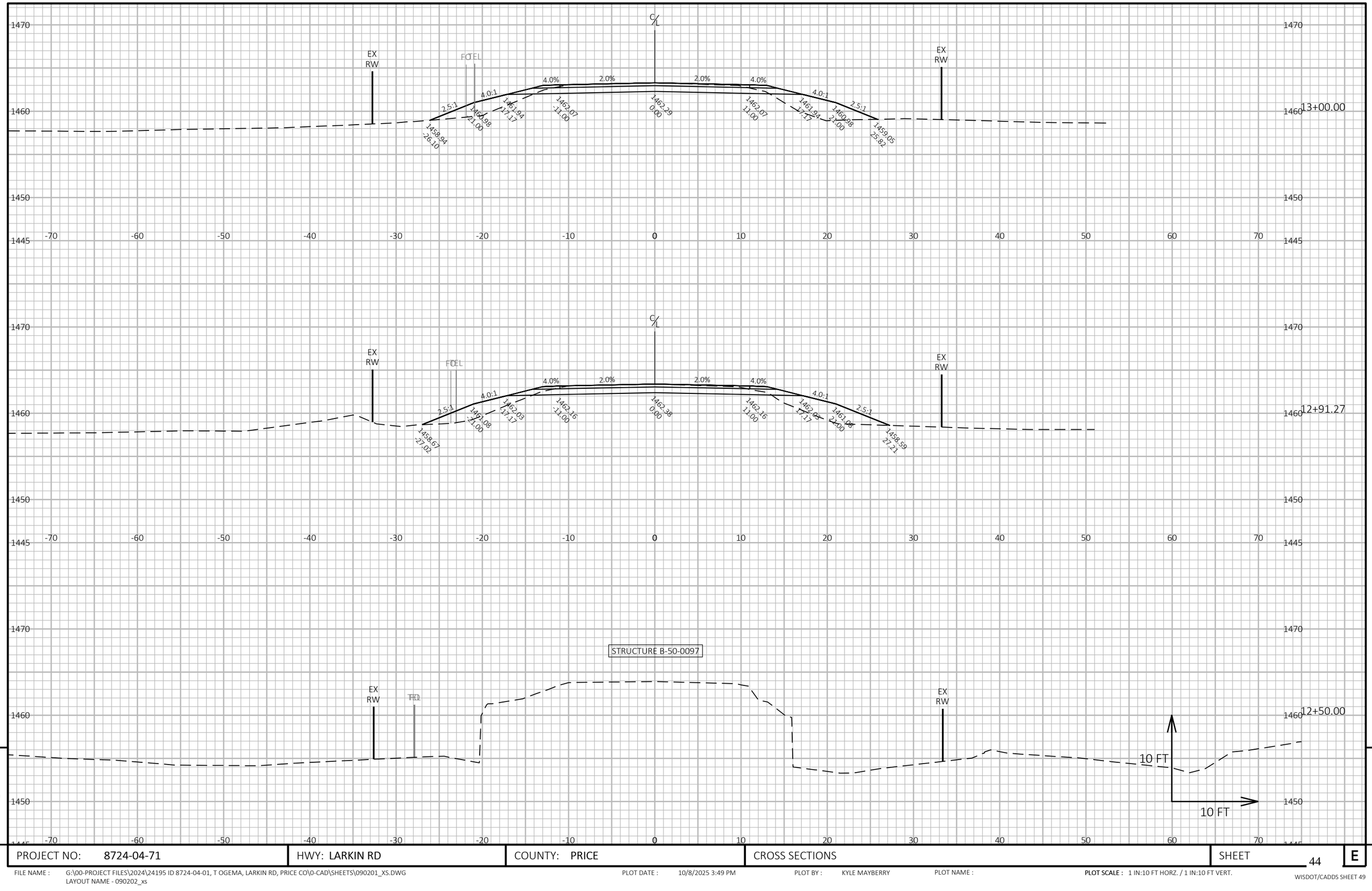
PROJECT NO: 8724-04-71	HWY: LARKIN RD	COUNTY: PRICE	CROSS SECTIONS	SHEET 43	E

FILE NAME : G:\00-PROJECT FILES\2024\24195 ID 8724-04-01, T OGEMA, LARKIN RD, PRICE CO\0-CAD\SHEETS\090201_XS.DWG
LAYOUT NAME - 090201_xs

PLOT DATE : 10/8/2025 3:49 PM PLOT BY : KYLE MAYBERRY PLOT NAME : **PLOT SCALE :** 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.



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9

PROJECT NO: 8724-04-71	HWY: LARKIN RD	COUNTY: PRICE	CROSS SECTIONS	SHEET 44	E

STA. 13+16.27
END PROJECT
MATCH EXISTING

C/L

EX RW

FO

TEL

EX RW

13+16.27

10 FT

10 FT

1470

1460

1450

-70 -60 -50 -40 -30 -20 -10 0 10 20 30 40 50 60 70

9

9

PROJECT NO: 8724-04-71	HWY: LARKIN RD	COUNTY: PRICE	CROSS SECTIONS	SHEET 45	E

FILE NAME : G:\00-PROJECT FILES\2024\24195 ID 8724-04-01, T OGEMA, LARKIN RD, PRICE CO\0-CAD\SHEETS\090201_XS.DWG
LAYOUT NAME - 090203_xs

PLOT DATE : 10/8/2025 3:49 PM PLOT BY : KYLE MAYBERRY PLOT NAME : **PLOT SCALE :** 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49

Wisconsin Department of Transportation

Dedicated people creating transportation solutions
through innovation and exceptional service.

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