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Final Section 4(f) Evaluation

US 51 Corridor Study

**I-39/90 to US 12/18 (Madison South Beltline)
Dane County**

The Wisconsin Department of Transportation

Date Approved: 17 August 2021

DocuSigned by:

Bethaney Bacher-Gresock

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Federal Highway Administration

FHWA

**Project Sponsor:
Wisconsin Department of Transportation
Southwest Region**

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1. Introduction

The U.S. Department of Transportation's Section 4(f) law (23 USC 138 & 49 USC 303) states that federal funds may not be approved for projects that use land from a significant publicly owned park, recreation area, wildlife or waterfowl refuge, or public or private historic site unless it is determined that there is no feasible and prudent alternative to the use of land from such properties, and the action includes all possible planning to minimize harm to the property resulting from such use.

Section 4(f) regulations in 23 CFR 774.17 specify how the Federal Highway Administration (FHWA) is to determine whether a potential avoidance alternative is feasible and prudent.

- An alternative is not feasible if it cannot be built as a matter of sound engineering judgment.
- A six-factor analysis is used to determine if an avoidance alternative is not prudent. The avoidance alternative is not prudent if:
 1. It compromises the project to a degree that it is unreasonable to proceed with the project in light of its stated purpose and need.
 2. It results in unacceptable safety or operational problems.
 3. After reasonable mitigation, it still causes severe social, economic, or environmental impacts; severe disruption to established communities; severe disproportionate impacts to minority or low-income populations; or severe impacts to environmental resources protected under other federal statutes.
 4. It results in additional construction, maintenance, or operational cost of an extraordinary magnitude.
 5. It causes other unique problems or unusual factors. Examples of unique problems include unacceptable social, economic, or environmental impacts; serious community disruption; unacceptable safety or geometric problems; or excessive construction costs. An accumulation of these problems (as opposed to a single factor) may be a sufficient reason to use a Section 4(f) resource, but only if the problems are truly unique. Excessive cost alone will not necessarily prevent an alternative from being considered prudent.
 6. It involves multiple factors listed above, that while individually minor, cumulatively cause unique problems or impacts of extraordinary magnitude.

When a federally funded transportation project will result in a use of a Section 4(f) property, a Section 4(f) evaluation is prepared and approval by FHWA is required. The evaluation describes the Section 4(f) property, the proposed use of the property, avoidance and minimization alternatives, other impacts associated with the alternatives, coordination with the official(s) with jurisdiction, and measures to minimize harm. If the Section 4(f) analysis concludes that there is no feasible and prudent avoidance alternative, then FHWA may approve, from among the remaining alternatives that use Section 4(f) property, only the alternative that causes the least overall harm in light of the statute's preservation purpose.

Section 4(f) applies only to the actions of agencies within the U.S. Department of Transportation, including FHWA. While other agencies may have an interest in Section 4(f), FHWA is responsible for Section 4(f) applicability determinations, evaluations, findings, and overall compliance for highway projects.

2. Study Background

The United States Highway (US) 51 study area is located in south central Wisconsin in the southeast corner of Dane County. The area lies directly southeast of the city of Madison (Madison)(Figure 1). The study corridor begins at Interstate 39/90 (I-39/90) approximately 5 miles east of the city of Stoughton (Stoughton) and extends westward through downtown Stoughton and north along the west side of Stoughton. It continues north through a rural area and then through the village of McFarland (McFarland); the study corridor terminates at US 12/18 (Madison South Beltline). The length of the study corridor is 18.6 miles.

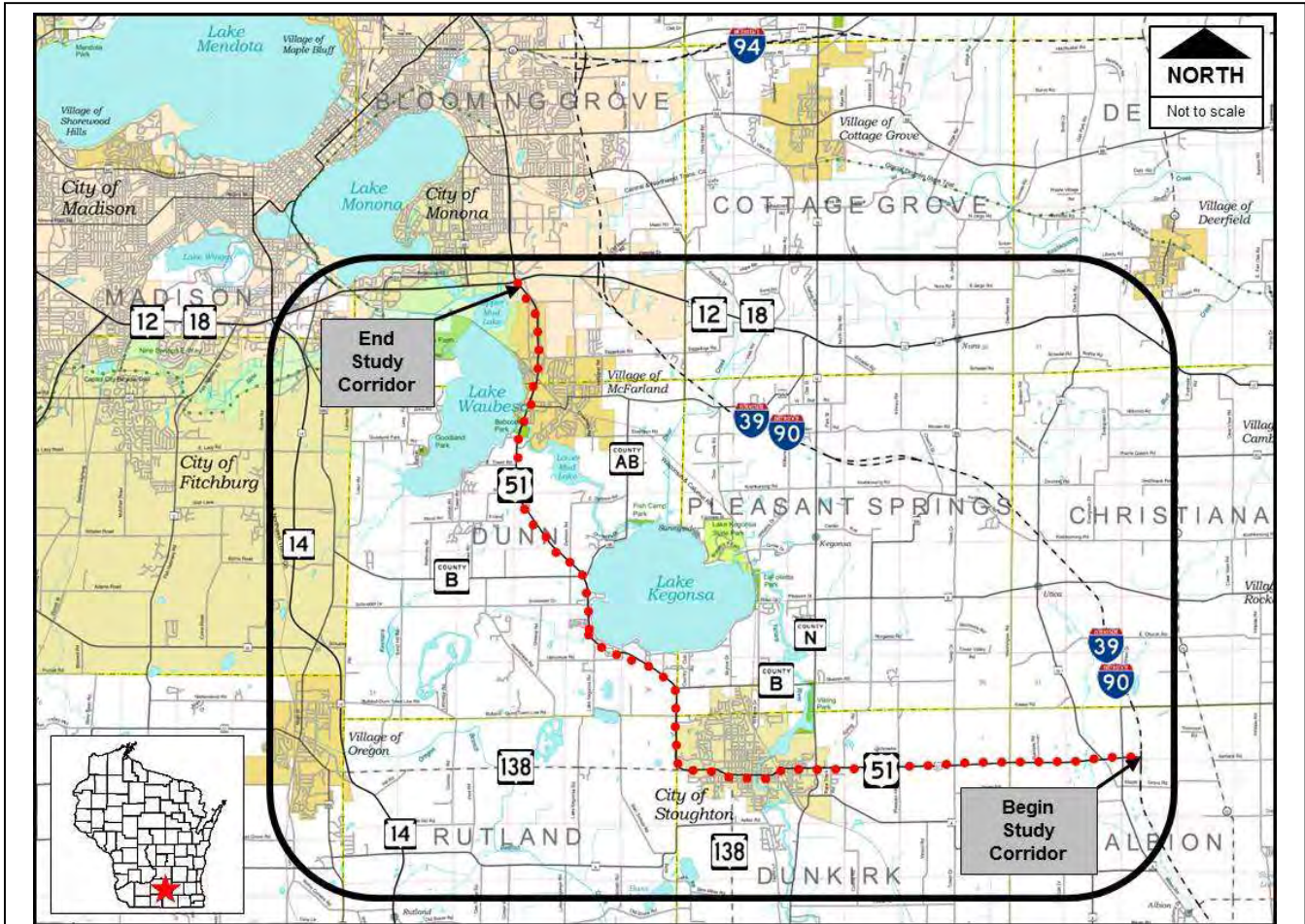


Figure 1 The US 51 Study Area in Dane County, Wisconsin

The US 51 Corridor Study is an ongoing study to evaluate alternatives that will improve safety and congestion along the corridor and address needs of bicyclists and pedestrians. The study has progressed from a Needs Assessment in 2002, to the evaluation of multiple improvement alternatives as part of an Environmental Impact Statement (EIS) from 2006 to 2013, and downscoped to an Environmental Assessment (EA) in 2016. Following the previous environmental studies, Wisconsin Department of Transportation (WisDOT) and FHWA agreed an EA would be prepared to address needs along the Stoughton to McFarland corridor to determine significance of impacts. The EA and this Section 4(f) Evaluation are being completed under WisDOT Project ID 5845-06-03. The previous environmental study phase was conducted under WisDOT Project ID 5845-06-02 and the US 51 Corridor Study correspondence may reference either of these project ID numbers.

US 51 Existing Conditions

The US 51 study corridor is an important commuter route in southeastern Dane County. It connects to I-39/90 and US 12/18, which are both National Highway System (NHS) routes and Connections 2030 Backbone routes. NHS routes are important to the nation’s economy, mobility, and defense. Connections 2030 Backbone (and Connector) routes are identified in Wisconsin’s Connections 2030 Long-Range Multimodal Transportation Plan adopted October 2009 and signify Wisconsin’s most important highways. While US 51 is not an NHS route, a Backbone or Connector route, the US 51 study corridor is an important commuter route in southeastern Dane County that connects I-39/90 and US 12/18. US 51 functions as a principal arterial for most of the corridor except for the 5.7-mile section east of Stoughton from I-39/90 to County N, which is classified as a minor arterial.

Figure 2 shows the functional classification, number of lanes, and posted speeds along the study corridor. US 51 has a variety of roadway cross sections but is a 2-lane roadway for more than 75 percent of the 18.6-mile study corridor. There are two 4-lane sections; one in Stoughton is 1.2 miles long and located west of the downtown

area. The other 4-lane section begins at Exchange Street, 0.4 miles south of the McFarland village limits, and continues north for the remaining 3 miles to US 12/18. The northernmost 2-mile portion of the study corridor is a 4-lane expressway. The 4-lane urban section in McFarland that is adjacent to Babcock Park is an undivided section with 12-foot lanes.

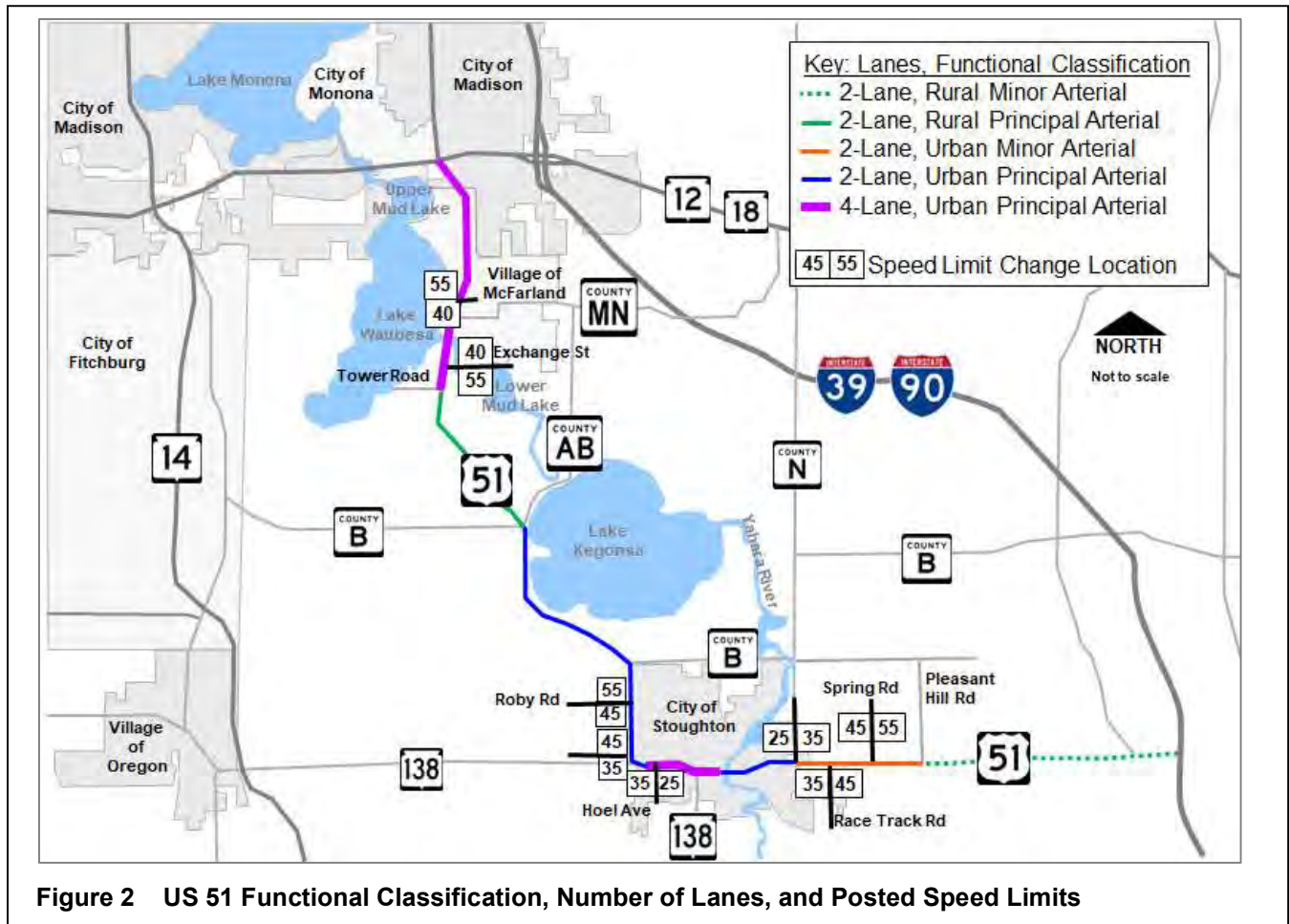


Figure 2 US 51 Functional Classification, Number of Lanes, and Posted Speed Limits

3. Purpose and Need and Range of Alternatives

The purpose of this project is to provide a safe and efficient transportation system in the US 51 corridor to serve present and long-term travel demand while minimizing disturbance to the environment. This will be obtained by working to address existing safety conditions, accommodating travel demand, addressing existing pavement conditions, improving bicycle and pedestrian accommodations, and considering corridor preservation and long-term planning measures.

The following five contributing factors of need support the purpose of improvements to the US 51 corridor:

1. Address Existing Safety Conditions:
 Safety-related concerns are related to unsafe conditions at intersections, the lack of passing opportunities, travel speeds over the posted limit, the difficulty experienced by motorists getting on and off US 51 safely, and the number of crashes or “near misses.”
2. Accommodate Travel Demand:
 Based on expected traffic volumes and the existing roadway capacity, together producing unacceptable levels of congestion, there is a need for improvements on portions of US 51.
3. Address Existing Pavement Condition:
 For the majority of the corridor, the underlying pavement is near or has surpassed its useful life.

4. **Improve Bicycle and Pedestrian Accommodations:**
The lack of bicycle facilities in the rural areas and the lack of, or discontinuous, network of the existing bicycle and pedestrian facilities in urban areas limit the use of non-motorized travel modes in the US 51 study corridor.
5. **Corridor Preservation and Long-Term Planning:**
US 51 serves as one of the major connections between Stoughton, McFarland, and Madison. Growth in these communities and the greater Madison area makes US 51 an important corridor to preserve mobility and safety.

The alternatives considered include a No Build Alternative, Alternative A, Alternative B, and Alternative H. Refer to the Alternatives Comparison Matrix in Appendix C for a summary of the environmental impacts and costs for each alternative.

No Build Alternative

The No Build Alternative serves as a “baseline” from which to compare the alternatives under consideration. Under the No Build Alternative, no intersection improvements, reconstruction, or capacity improvements would be made to the existing US 51 corridor. Independent of the No Build Alternative or any build alternative, there are currently programmed projects (a pavement replacement project and four roundabouts) planned for construction within the corridor. Under the No Build Alternative, the existing road would bear future traffic increases, congestion, roadway deficiencies, and worsening pavement conditions with effects on safety, congestion, mobility, and operational characteristics. The No Build Alternative includes the cost of routine maintenance through the design year and would have no associated right of way (R/W) impacts. The preliminary total cost estimate for the No Build Alternative is \$28 million in 2016 dollars.

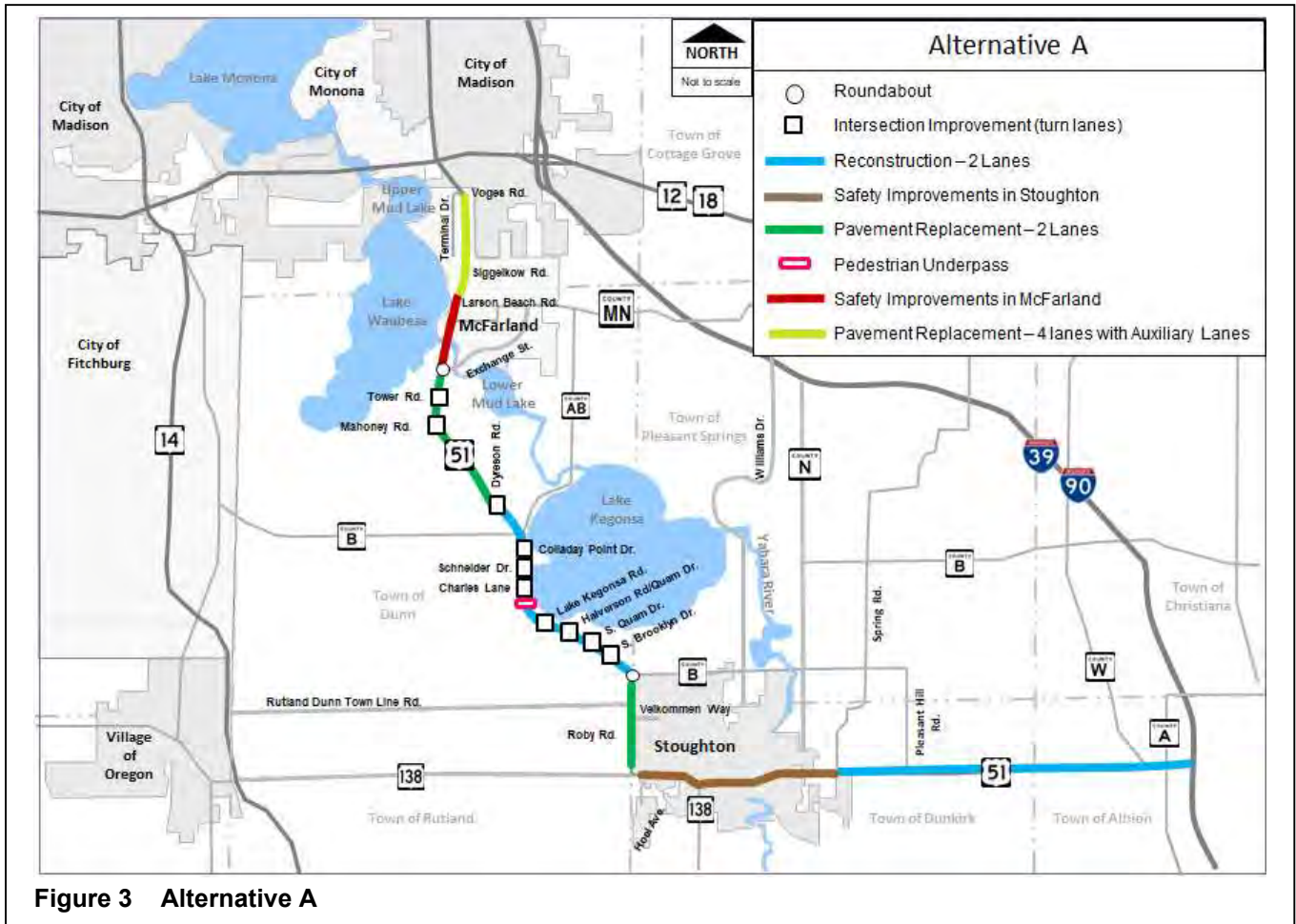
Alternative A–Low Build

Alternative A is considered the lower cost and lower impact alternative. Alternative A has seven main components to the improvement between I-39/90 and US 12/18.

1. Reconstruction of 2-lane US 51 east of Stoughton.
2. Safety improvements in Stoughton.
3. Safety improvements in McFarland.
4. Two roundabouts and other intersection improvements between Stoughton and McFarland.
5. Reconstruction of 2-lane US 51 from County B (east) to Dyreson Road.
6. Pavement replacement in multiple sections between Stoughton and McFarland.
7. Pavement replacement between Larson Beach Road and Terminal Drive/Voges Road in McFarland, Siggelkow Road interchange ramp improvements and the addition of an auxiliary lane in each direction north of Siggelkow Road.

Alternative A also provides bicycle and pedestrian accommodations. Bicycle accommodations would be provided on paved shoulders along the rural sections of the corridor. Improvement of pedestrian accommodations would be provided by the replacement of the pedestrian box culvert near Charles Lane to serve residents of Bay View Heights, a manufactured home community, as a pedestrian access to the area east of US 51 and to Lake Kegonsa. Minor pedestrian improvements would be provided by revising the crosswalk pavement marking at Burma Road in McFarland to provide pedestrian refuge on the existing median near Babcock Park.

The preliminary total cost estimate for Alternative A is \$99 million (in 2016 dollars). Alternative A partially addresses the project’s purpose and need and is anticipated to meet the federal fiscal constraint requirement. Figure 3 provides an overview of Alternative A.



Alternative B—Four-Lane Expansion

Alternative B has a higher financial cost, higher real estate and relocation impacts, and greater environmental impacts than the other alternatives. Alternative B addresses the project’s purpose and need, but it does not meet the federal fiscal constraint requirement. Alternative B was developed to preserve the US 51 corridor functionality, address future projected travel demand by increasing capacity, improve safety, correct roadway deficiencies, provide bicycle accommodations throughout and pedestrian accommodations in urban areas, and address pavement conditions.

Alternative B has six main components that would include the following:

1. Reconstruction of 2-lane US 51 east of Stoughton.
2. Safety improvements in Stoughton.
3. Construct 4-lane expansion around Stoughton (Stoughton Bypass).
4. Rural 4-lane expansion (Stoughton to McFarland).
5. Urban 4-lane reconstruction in McFarland.
6. Pavement replacement between Larson Beach Road and Terminal Drive/Voges Road in McFarland, Sigelkow Road interchange ramp improvements, and addition of an auxiliary lane in each direction north of Sigelkow Road.

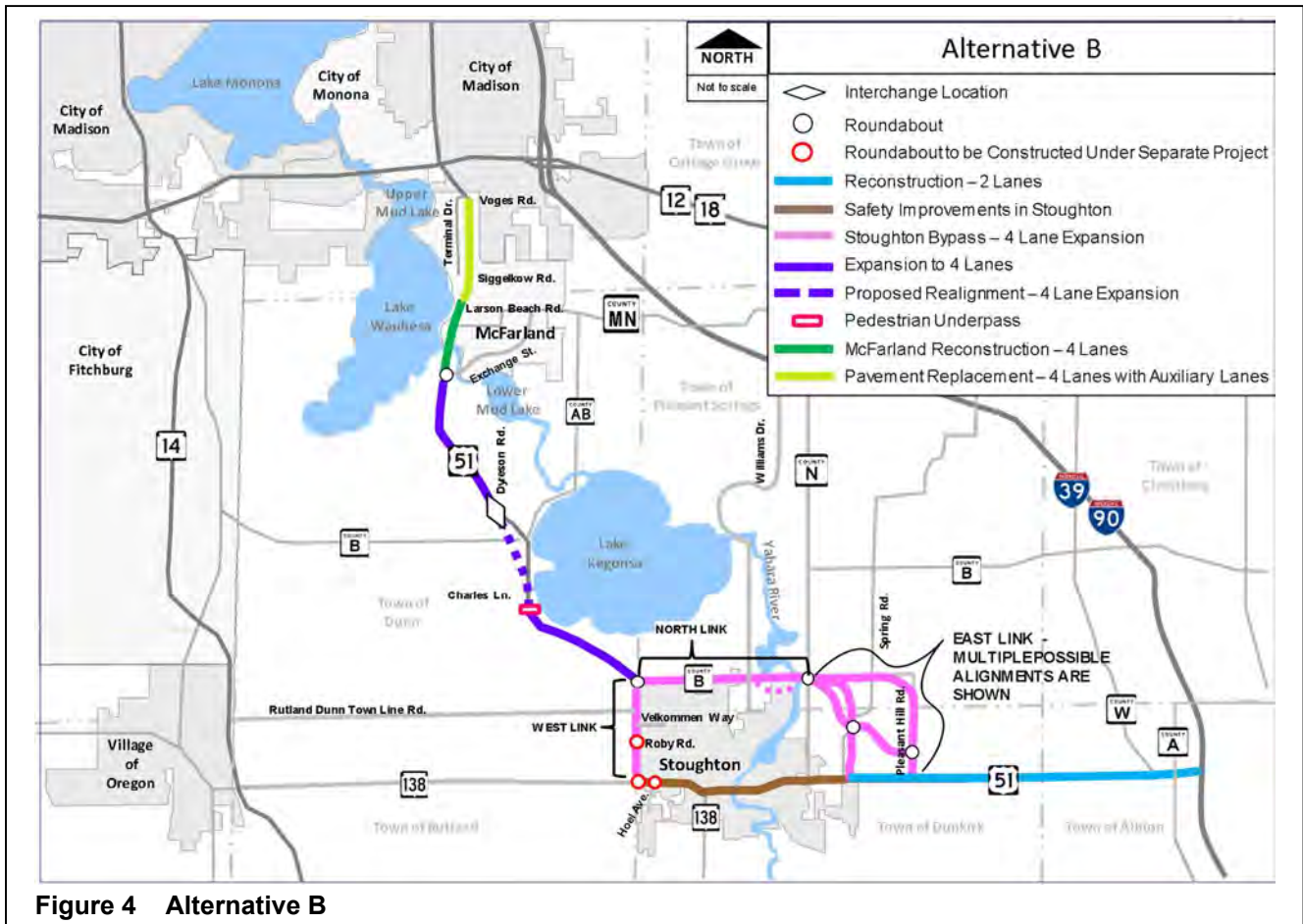
Alternative B also provides bicycle and pedestrian accommodations, as follows:

1. Bicycle accommodations are provided on US 51 and the Stoughton Bypass on paved shoulders in rural areas. For the west link of the Stoughton Bypass (a 4-lane urban section with a curbed median), bicyclists could take a lane as allowed by statute, ride on the sidewalk as allowed by Stoughton ordinance, or ride

on the paved shoulder. In McFarland, from the Exchange Street roundabout to Larson Beach Road, bicyclists can take a traffic lane as allowed by statute. Bicyclists can also use the existing path in Babcock Park. A connection to the path in Babcock Park will be provided and the path adjacent to the west side of the Yahara River bridge will be reconstructed with the bridge replacement structure.

2. Accommodations for sidewalk would be provided wherever the reconstructed roadway has an urban section with curb and gutter. Pedestrian crossings would be improved in McFarland where there is a lack of pedestrian refuge at signalized intersections and at the unsignalized Burma Road crossing adjacent to Babcock Park.
3. Public comments identified the need for one pedestrian crossing in the rural section between Stoughton and McFarland at the existing pedestrian culvert beneath US 51 immediately south of the rock cut near Charles Lane. The structure would be reconstructed as part of Alternative B to serve residents of the Bay View Heights community as a pedestrian access to businesses east of US 51 and to Lake Kegonsa without having to cross US 51 at grade. The pedestrian culvert would be reconstructed to an appropriate size with lighting and other safety features.

The preliminary total cost estimate for Alternative B is \$304 to \$321 million (in 2016 dollars). Alternative B does not meet the federal fiscal constraint requirement because WisDOT has determined that based on statewide priorities, Alternative B would likely not receive funding for the next major action to advance the project. Figure 4 provides an overview of Alternative B.



Alternative H—Hybrid Alternative (Preferred Alternative)

Alternative H is the preferred alternative and is the “hybrid” alternative that combines aspects of Alternatives A and B. Alternative H has six main components that would include the following:

1. Reconstruction of 2-lane US 51 east of Stoughton (same as Alternatives A and B).
2. Reconstruction of existing US 51 through downtown Stoughton (different from Alternatives A and B).
3. Urban 4-lane expansion along the west side of Stoughton (same as Alternative B West Link of Stoughton Bypass).
4. Reconstruction of rural 2-lane US 51 (Stoughton to McFarland) with intersection improvements (similar to Alternative A).
5. Urban 4-lane reconstruction in McFarland (same as Alternative B).
6. Pavement replacement between Larson Beach Road and Terminal Drive/Voges Road in McFarland, Sigelkow Road interchange ramp improvements, and addition of an auxiliary lane in each direction north of Sigelkow Road (same as Alternatives A and B).

Alternative H also provides bicycle and pedestrian accommodations, as follows:

1. In rural areas where pavement reconstruction or pavement replacement would be provided, bicycle accommodations would be provided on the paved shoulders.
2. On-street bicycle accommodations are also provided in the urban areas of Stoughton, where possible without taking additional R/W. Because of the constrained and highly developed downtown environment, historic districts that border US 51, and Stoughton’s desire to retain US 51 on-street parking through the Central Business District (CBD), bike accommodations do not fit everywhere along US 51. In those sections, accommodations would be provided by signed routes on streets one block north or south of and parallel to US 51.
 - a. From Spring Road to Amundson Parkway, 5-foot bike lanes are provided.
 - b. From Amundson Parkway to the railroad crossing, 4- to 5-foot bike lanes are provided.
 - c. From the railroad crossing to 5th Street, minimum bike accommodations are provided with a shared bike and parking lane.
 - d. From 5th Street to the Yahara River, bicycles would use signed parallel routes on residential streets.
 - e. Bikes would use US 51 to cross the Yahara River.
 - f. From Page Street to Wisconsin State Highway (WIS) 138(south)/Van Buren Street, bicycles would use signed parallel routes on residential streets.
 - g. From WIS 138 (south)/Van Buren Street to WIS 138 (west), bicycles would use signed parallel routes on residential streets.
3. On the west side of Stoughton, from WIS 138 (west) to County B (east), the proposed typical section would be expanded from a 2-lane to a 4-lane, high-speed urban section with a curbed median, curb and gutter along the outside paved shoulders, 10-foot-wide sidewalk on both sides, and on-street bicycle accommodations on 8-foot outside paved shoulders.
4. In McFarland, from the Exchange Street roundabout to Larson Beach Road, bicyclists can take a traffic lane as allowed by statute. Bicyclists can also use the existing path in Babcock Park. A connection to the path in Babcock Park will be provided and the path adjacent to the west side of the Yahara River bridge will be reconstructed with the bridge replacement structure.
5. Sidewalk for pedestrians would be constructed to be continuous wherever the proposed roadway has an urban section with curb and gutter. In the less developed areas with a proposed urban roadway cross section, Alternative H could provide grading only for future sidewalk construction. An example area where this might be considered is in the 1,500-foot section between Spring Road and 1,000 feet east of County N in Stoughton. The decision to forgo sidewalk and just provide the grading would be made during final design following consultation with the local municipality.
6. Pedestrian crossings would be improved in McFarland where there is a lack of pedestrian refuge at signalized intersections and at the unsignalized Burma Road crossing adjacent to Babcock Park.
7. In Stoughton, the sidewalk width will be increased where deficient.

8. Pedestrian crossings in Stoughton and McFarland will have pavement marking and WisDOT will work with the municipalities during final design to provide acceptable signage and lighting at each pedestrian crossing.
9. As described for Alternatives A and B, the existing pedestrian culvert beneath US 51 immediately south of the rock cut near Charles Lane would be reconstructed.

The preliminary total cost estimate for Alternative H is \$203.4 million in in year of expenditure (YOE) dollars. The YOE cost includes estimated costs from completion of the environmental document through construction at the end of 2029. A YOE cost has only been calculated for the preferred alternative (Alternative H). Alternative H partially addresses the project’s purpose and need and meets more project need elements than Alternative A. Alternative H has a lower cost and fewer impacts than Alternative B. Alternative H is anticipated to meet the federal fiscal constraint requirement. Figure 5 provides an overview of Alternative H.

Because Babcock Park is located in McFarland, Table 1 was prepared to compare how the build alternatives address the project needs in the McFarland area.

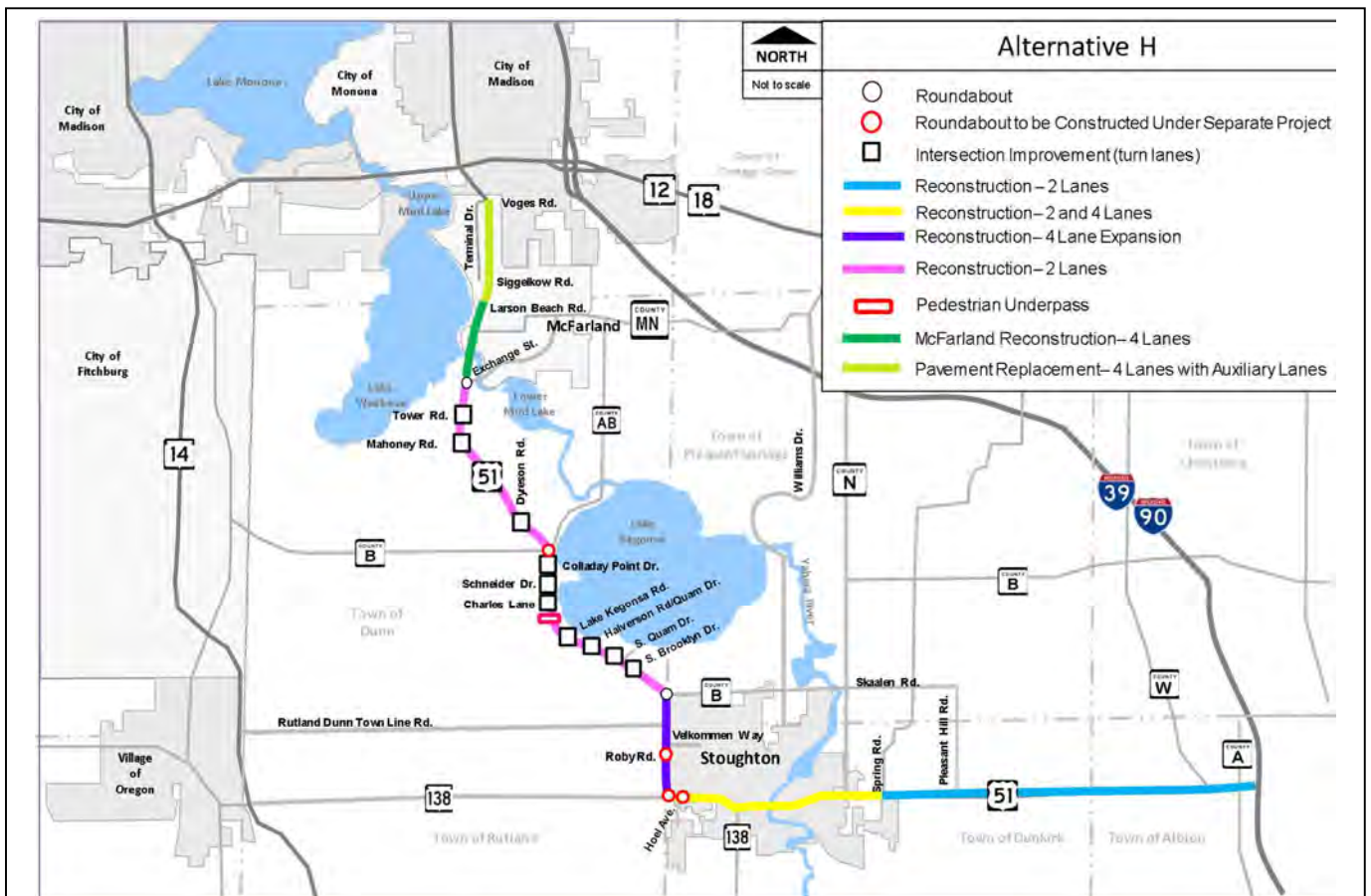


Figure 5 Alternative H

Table 1 Comparison of How the Build Alternatives Address Needs in the McFarland Area

Need Criteria in McFarland Area	Alternative A: Low Build	Alternative H: Hybrid ¹ (Preferred Alternative) and Alternative B: Four-Lane Expansion
Improve Safety	<p>Partially Addressed</p> <p>Alternative A provides a minor safety improvement by revising the crosswalk pavement markings at Burma Road. It does not improve safety because it does not provide turn lanes on US 51 in the vicinity of the park, which decreases both safety and mobility because turning vehicles must wait in through traffic lanes for the opportunity to make the turn.</p>	<p>Partially Addressed</p> <p>These alternatives address multiple safety needs because they provide a right-turn lane for southbound vehicles slowing to turn into the Babcock Park boat launch parking lot and a median refuge and turn lane for northbound vehicles turning left to the park, both at the boat launch parking lot and at Burma Road where driveway access to the park campground is provided. The proposed median for left-turning vehicles also provides refuge for pedestrians crossing US 51 between the boat launch parking lot and the overflow parking lot.</p>
Accommodate Travel Demand	<p>Partially Addressed</p> <p>Desirable Level of Service (LOS) conditions are expected for the signalized intersections in McFarland; however, the southbound left-turn movement from US 51 to Farwell Street (County MN) has projected queues extending past the existing turn-bay length and into through traffic.</p>	<p>Fully Addressed</p> <p>Desirable LOS conditions are expected for the signalized intersections in McFarland and the alternatives provide a second southbound left-turn lane at Farwell Street (County MN) to eliminate queues extending into the through traffic on US 51 at this key intersection.</p>
Address Existing Pavement Condition	<p>Partially Addressed</p> <p>No pavement improvements are proposed within the 1-mile urban section of US 51 in McFarland. Pavement replacement is included in the expressway section between Larson Beach Road and Terminal Drive/Voges Road.</p>	<p>Fully Addressed</p> <p>Pavement is replaced or reconstructed through the entire project corridor.</p>
Improve Bicycle and Pedestrian Accommodations	<p>Partially Addressed</p> <p>Bicycle accommodations are provided on paved shoulders in the rural section. Bicycle and pedestrian accommodations are not addressed in the McFarland urban section because the pavement, curb and gutter, and sidewalks are not proposed for reconstruction. Minor pedestrian improvements would be provided by revising the crosswalk pavement marking at Burma Road in McFarland to provide pedestrian refuge on the existing median near Babcock Park.</p>	<p>Partially Addressed</p> <p>Bicycle accommodations are provided on paved shoulders in the McFarland rural section and where possible in the urban section. Alternatives provide pedestrian accommodations along US 51 in the McFarland urban section.</p>
Corridor Preservation and Long-Term Planning	<p>Partially Addressed</p> <p>Provides minor safety improvements in the urban section and mobility improvements only in the expressway section of McFarland.</p>	<p>Partially Addressed</p> <p>Maintains a functional 4-lane principal arterial corridor through McFarland by improving mobility and providing multiple safety improvements.</p>

¹ Alternatives B and H have the same impacts to Babcock Park.

4. Description of Preferred Alternative Section 4(f) Properties

The Section 4(f) properties located within the limits of Alternative H are shown on Figure 6 and described in the following paragraphs.

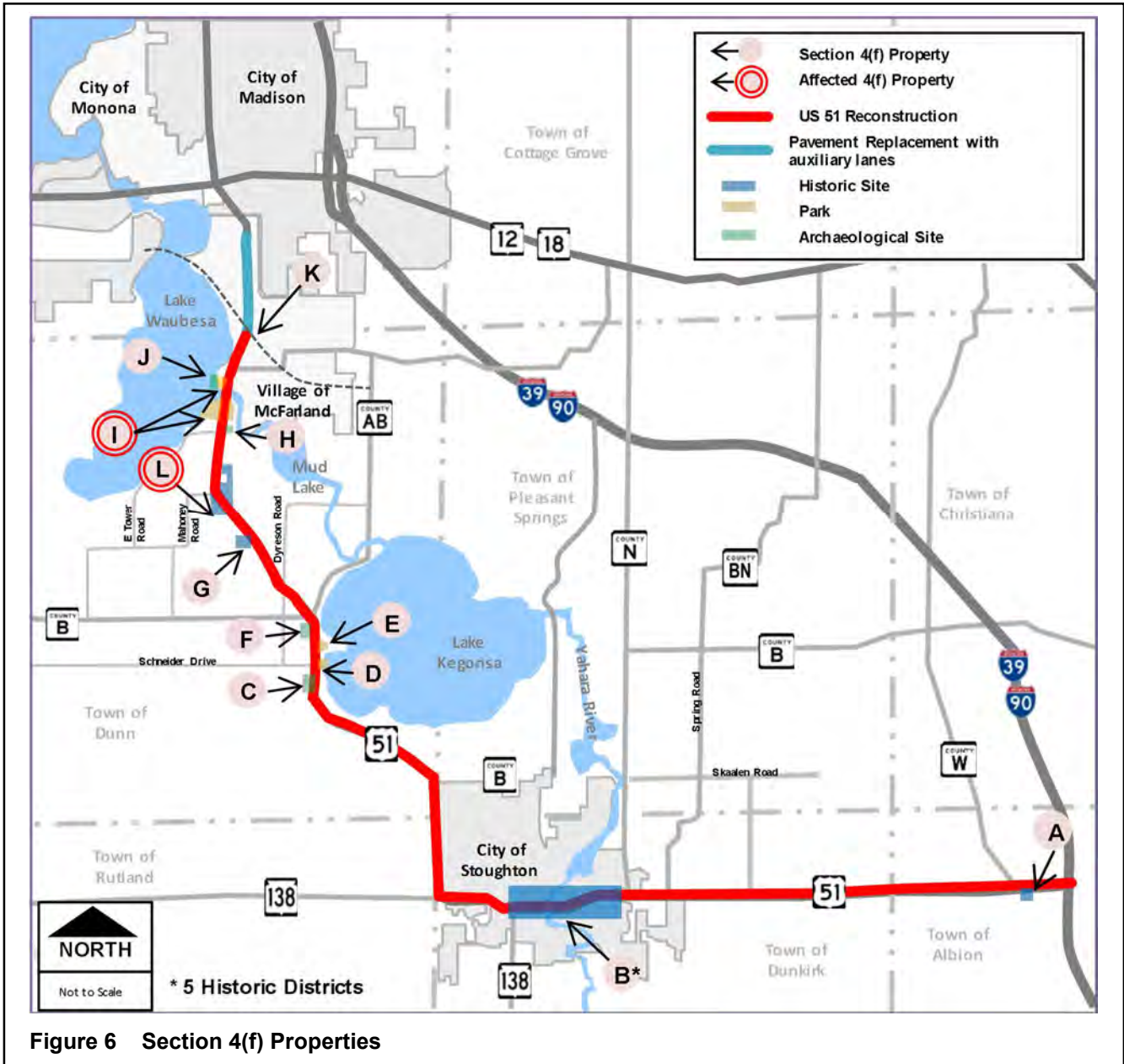


Figure 6 Section 4(f) Properties

A. Historic Maple Grove School (Site A)

Maple Grove School is located east of Stoughton near I-39/90 at the intersection of US 51 and County W/Maple Grove Road. According to a determination of eligibility (DOE) completed in 1988, the Maple Grove School was previously determined eligible for the National Register of Historic Places (NRHP) under Criterion A. FHWA's requirements for Section 4(f) do not apply because no R/W or permanent or temporary limited easement will be acquired from the property and the project will not substantially impair the characteristics that qualify the property for the NRHP.

B. Historic Districts in Stoughton (Site B*, Five Historic Districts)

The project's proposed bicycle accommodations on US 51 extend through downtown Stoughton but are routed off US 51 onto streets one block north or south of and parallel to US 51 for several blocks to avoid impacts to the historic districts that would result from roadway widening to provide on-street bike accommodations. Rerouting would use signage only, no pavement changes or widening is proposed. West of Van Buren Street, bikes will be routed north of US 51 on Van Buren Street for two blocks and west on Jackson Street to US 51. A portion of this rerouting along Van Buren Street and Jackson Street is within the Northwest Side Historic District. Between Van Buren Street and Page Street, bikes will be routed one block south of US 51 along Jefferson Street. A portion of the rerouting along Jefferson Street is within the Southwest Side Historic District. Between Page Street and Water Street, bike accommodation is back on US 51. Between Water Street and 6th Street, bikes will be routed either one block south of US 51 to Jefferson Street or one block north to Washington Street. This measure avoids impacts to three historic districts (Northwest Side, Southwest Side, and Main Street Commercial) by avoiding the need to widen the pavement along US 51 to accommodate bicycles.

FHWA's requirements for Section 4(f) do not apply to the project's use of any of these historic districts because no R/W or permanent easement will be acquired, and the project will not substantially impair the characteristics that qualify the districts for the NRHP.

Northwest Side Historic District

This district is bounded roughly by US 51 (Main Street) to the south, the Yahara River and Grant Street to the east, Jackson, Roy, and Taft Streets to the north, and Van Buren Street to the west. The Northwest Side Historic District was listed in the NRHP in 1998 under Criterion C: Architecture, as a relatively intact concentration of historic houses constructed between 1850 and 1940. The existing US 51 R/W in the Northwest Side Historic District is 66 feet wide. The existing back of sidewalk is 0.5 feet from the edge of existing R/W. The proposed back of sidewalk will be at the same location and no R/W will be acquired.

Southwest Side Historic District

This district is bounded roughly by Oak Street to the south, South Page Street to the east, West Main Street to the north, and South Monroe Street to the west. The Southwest Side Historic District was listed in the NRHP in 1998 under Criterion C: Architecture, as a concentration of significant examples of popular nineteenth- and early-twentieth-century architectural styles. The existing US 51 R/W in the Southwest Side Historic District is 66 feet wide. The existing back of sidewalk is 0.5 feet from the edge of existing R/W. The proposed back of sidewalk will be at the same location and no R/W will be acquired.

Main Street Commercial Historic District

This district is located along Main Street between the Yahara River and Forest Street. The Main Street Commercial Historic District was listed in the NRHP in 1982 under Criterion C: Architecture, as a cohesive collection of buildings comprising Stoughton's historic commercial center. Existing sidewalk is located between curb lines and buildings. The existing US 51 R/W in the Main Street Commercial Historic District is 66 feet wide and includes 8-foot-wide sidewalks behind each curb line. The proposed sidewalk will be replaced within the limits of the existing R/W and no R/W will be acquired.

Depot Hill Historic District

This district is located along East Main Street between South 5th Street and the Chicago, Milwaukee, St. Paul & Pacific railroad tracks. The Depot Hill Historic District was determined eligible for listing in the NRHP in 1998 under Criterion A: History, for its association with history of industry and transportation in Stoughton in the late nineteenth and early twentieth centuries. Existing sidewalk is located between curb lines and buildings. The existing R/W on US 51 in the Depot Hill Historic District is approximately 70 feet wide. The back of existing sidewalk is 2.5 feet from the edge of existing R/W. The proposed back of sidewalk will be at the same location and no R/W will be acquired.

East Side Historic District

This district is bounded roughly by Vernon Street to the south, South and North Henry Streets to the east, Ridge Street to the north, and South Academy Street to the west. The East Side Historic District was listed in the NRHP in 1997 under Criterion C: Architecture, as a collection of houses constructed between 1880 and 1940 that represent popular nineteenth- and early-twentieth-century architectural styles. The existing R/W on

US 51 in the East Side Historic District is 66 feet wide. The existing back of sidewalk is 0.5 feet from the edge of existing R/W. The proposed back of sidewalk will be at the same location and no R/W will be acquired.

C. Barber Campsite, 47DA0107 (Site C)

This archaeological site is located along the west side of US 51 between Charles Lane and Schneider Drive in the town of Dunn (Dunn). The site was determined to be eligible for the NRHP because it contains *in-situ* cultural features and a high density of archaeological materials. Adverse effects are anticipated and data recovery will be completed at this site. Documentation for Consultation has been completed and a Memorandum of Agreement (MOA) has been executed. FHWA requirements for Section 4(f) do not apply to Site #47DA0107 (Barber Campsite) because the exception in CFR 774.13(b) applies to the site. Section 4(f) applies to archeological sites that are on or eligible for the National Register of Historic Places (NRHP) and that warrant preservation in place. Section 4(f) does not apply if FHWA determines that the archeological resource is important chiefly because of what can be learned by data recovery and has minimal value for preservation in place, and the State Historic Preservation Office (SHPO), Tribal Historic Preservation Office (THPO), and Advisory Council on Historic Preservation (ACHP) (if participating) does not object to this determination. Agreement with the determination is documented in Section III, question 13 of the March 23, 2016 Final Documentation for Consultation. An excerpt from the Final Documentation for Consultation is provided as Appendix D. The Section 106 form and MOA are included with the project's environmental document. Other documentation can be requested from WisDOT Southwest Region.

D. Lincoln Point Park (Site D)

This 0.37-acre Dunn park is located east of Barber Drive, between Lake Kegonsa and Barber Drive. The park is used for stormwater drainage and access to Lake Kegonsa. Because of its status as a public park, Lincoln Point Park qualifies for protection under Section 4(f). FHWA's requirements for Section 4(f) do not apply to the project's use of the park property because no R/W or permanent or temporary limited easement will be acquired from the property and the project will not affect the use of the property.

E. Colladay Point Park (Site E)

Colladay Point Park is a 7-acre Dunn park located on the east side of US 51 just west of Lake Kegonsa and south of County B/AB. Because of its status as a public park, Colladay Point Park qualifies for protection under Section 4(f). Dunn indicated the park is used primarily for trail walking and hiking. To avoid impacts to the park during and after construction, the beam guard and a retaining wall will be constructed outside of the park boundary. FHWA's requirements for Section 4(f) do not apply to the project's use of the park property because no R/W or permanent or temporary limited easement will be acquired from the property and the project will not affect the use of the property.

F. Colladay Mound, 47DA0105 (Site F)

This cataloged burial mound site is located along the west side of US 51 between Schneider Drive and County B/AB in Dunn. A burial mound was identified at the site and the site contains *in situ* cultural features and a high density of archaeological materials. FHWA's requirements for Section 4(f) do not apply to the project's use of the historic property because no R/W or permanent or temporary limited easement will be acquired from the property and the project will not substantially impair the characteristics of the property.

G. Historic Olson-Hemsing Farmstead (Site G)

Olson-Hemsing Farmstead is located along the west side of US 51 between Dyreson Road and Mahoney Road in Dunn. The Olson-Hemsing Farmstead contains 12 historic resources and the property's period of significance is c.1905 to c.1970. Because the Olson-Hemsing Farmstead is a good local representative of the typical evolution of an early-twentieth-century tobacco farm to a mid-twentieth-century dairy farm, the property is considered eligible for listing under Criterion C: Architecture as a distinct property type. FHWA's requirements for Section 4(f) do not apply to the project's use of the historic property because no R/W or permanent or temporary limited easement will be acquired from the property and the project will not substantially impair the characteristics that qualify the property for the NRHP.

H. Bird Effigy, 47DA0480 (Site H)

This uncatalogued effigy mound site is located near the intersection of US 51 and Exchange Street in McFarland. A burial mound was not identified at the site. The site does contain *in situ* cultural features and a high density of archaeological materials. FHWA's requirements for Section 4(f) do not apply to the project's use of the historic property because no R/W or permanent or temporary limited easement will be acquired from the property and the project will not substantially impair the characteristics of the property.

I. Babcock Park (Site I)

Babcock Park is a 16-acre Dane County park located in and directly south of McFarland. Alternative H (preferred alternative) requires an estimated 0.5 acres of fee R/W or approximately 3.1 percent of the park area in addition to 2.9 acres of temporary limited easement. Alternative B would have the same impacts as Alternative H. Impacts to the park would be avoided with the No Build Alternative and Alternative A. Because of its status as a public park, Babcock Park qualifies for protection under Section 4(f).

J. Babcock Park Archaeological Site, 47DA1429 (Site J)

An archaeological site is located within Babcock Park, north of the Yahara River between the existing campsites and Lake Waubesa. The site was likely an open-air campsite village harboring Early Woodland and Middle Woodland occupations. FHWA's requirements for Section 4(f) do not apply to the project's use of the historic property because no R/W or permanent or temporary limited easement will be acquired from the property and the project will not substantially impair the characteristics of the property.

K. Lower Yahara River Trail (Site K)

Phase 1 of the Lower Yahara River Trail was completed in 2017 from the Capital City Trail to McDaniel Park in McFarland. This 2.5-mile section of trail includes a nearly 1-mile-long bridge and boardwalk along the north shore of Lake Waubesa. McFarland has nearly completed the trail from McDaniel Park to Elvehjem Road using existing infrastructure. Dane County is responsible for the next phase of trail planning and development from Urso Park in McFarland to Lake Kegonsa State Park. When completed, the Lower Yahara River Trail is expected to be approximately 11 miles long. The Lower Yahara River Trail is open to hiking and biking, and other forms of non-motorized transit. The trail is located along the north side of Taylor Road where it crosses US 51 in McFarland. Because of its status as a public recreational facility, the Lower Yahara River Trail qualifies for protection under Section 4(f). The proposed action would reconstruct the US 51 bridges over Taylor Road and the trail. The trail would be temporarily rerouted during bridge reconstruction and the detour route would be coordinated with McFarland during final design. FHWA's requirements for Section 4(f) do not apply to the project's use of the trail because no R/W or permanent or temporary limited easement will be acquired from the property and the temporary occupancy exception in CFR 774.13(d) applies to the proposed action's use of the trail. In an email dated November 11, 2019, Dane County provided concurrence with the proposed temporary detour of the trail during US 51 bridge reconstruction. Dane County further indicated detour routing should be coordinated with McFarland. The email from Dane County is provided in Appendix A.

L. Brost Addition to Mud Lake (Site L)

The Brost Addition to Mud Lake (Brost Addition) is approximately 68 acres located along the east and west sides of US 51 near Mahoney Road. The land is owned and operated by the Groundswell Conservancy and was acquired in part with Knowles-Nelson Stewardship Grant funds administered by the Wisconsin Department of Natural Resources (WDNR). The land is open to the public and qualifies for protection under Section 4(f) with use defined in the Draft Land Management Plan and grant document as:

1. For conservation and recreation purposes (Management Plan).
2. To protect, enhance, and restore wildlife habitat and natural communities (Project Purpose in the grant document).
3. To enhance opportunities for wildlife-based outdoor recreation (Project Purpose in the grant document).

The proposed action would require approximately 1.7 acres of fee R/W from the Brost Addition, with impacts along both sides of US 51. In addition, the Kegonsa Sanitary District (KSD) maintains a sanitary sewer force main along the east side of US 51 within an easement on the property. KSD has indicated it will relocate portions of the force main as a result of the US 51 improvements and the need for additional easement acquisition by the KSD is anticipated. The finding of *de minimis* impact for the Brost Addition is included as Appendix E. Letters from WDNR and Groundswell Conservancy providing concurrence with the *de minimis* impact finding are included with the correspondence in Appendix A. Coordination with WDNR and Groundswell Conservancy is ongoing and potential mitigation measures discussed to date that are under consideration consist of construction of an improved access and parking area, signage, potential water access enhancements and/or other property enhancements. Mitigation measures will be finalized following real-estate appraisals to determine total value of required mitigation.

5. Babcock Park

A. Type and Ownership of Section 4(f) Property

Babcock Park is a Dane County park located along and bisected by US 51 within McFarland and Dunn. The park is used for camping, picnicking, and fishing and has boat and canoe launch facilities. Because Babcock Park is a publicly owned park, it qualifies for protection under Section 4(f).

Dingell-Johnson grant money was used for portions of the 1993 Babcock Park Access Renovation Project. That project included construction and renovation of park facilities located south of the Yahara River on the west side of US 51. The facilities improved included parking lots, boat launch ramps and pier, dredging, toilet facilities, and an asphalt walkway. The US 51 project would result in a temporary use of real property that interferes with the park's authorized purpose under the Dingell-Johnson grant. All requirements relating to Dingell-Johnson Sport Fish Restoration Act of 1950, 64 Stat. 430, as amended 16 USC 777-777n; and 50 CFR Part 80-Administrative Requirements, Pittman-Robertson Wildlife Restoration and Dingell-Johnson Sport Fish Restoration Act amended July 24, 2008, will be satisfied independent of the Section 4(f) Evaluation.

B. Property Description

Babcock Park is a 16-acre Dane County park located both in and directly south of McFarland. The south village limits in the area of US 51 are formed by the Yahara River. Babcock Park is used for camping, picnicking, and fishing and has boat and canoe launch facilities. Babcock Park is located on the east side of Lake Waubesa at the outflow of the Yahara River. North of the river, the park is on the west side of US 51. South of the river, the park is located on both sides of US 51. Figure 7 shows the location of Babcock Park in relationship to US 51 and McFarland.



Figure 7 Location of Babcock Park

See Figure 8 for a map of the park features. The park's boat launch offers a fish cleaning facility and accessible fishing pier. The park also has a boat mooring dock and a shore fishing station. The park features a 25-unit campground with electricity supplied to all sites, a fully accessible restroom and shower, and a sanitary dump station. Figure 9 is a map of the campground features.

The Yahara River flows unimpeded from the Mendota Locks through Lake Monona and Lake Waubesa. The Lake Waubesa Dam, popularly known as the Babcock Park Lock and Dam, is located at the outlet of Lake Waubesa in the town of Dunn. Dane County constructed the 10-foot-high dam in 1938 to control lake levels

and aid navigation. The dam holds a very small hydraulic head, often less than 1 foot, and the dam is often open during the year because the water level is held up by the channel constriction downstream of the dam. The dam controls the water levels for Lake Monona and Lake Waubesa and continues to be owned and operated by Dane County.

Access is provided by an asphalt driveway from US 51 on the south end of the property leading to a parking lot adjacent to the office and shower building and via a second asphalt drive on Burma Road on the northern portion of the property providing access to the campground and sites. The roads are not connected with the campground road ending in a cul-de-sac on the south end.

Babcock Park has a seasonal campground with a basic operating (open for camping) schedule of May 1 to October 15. The potential for an additional two to four weeks of camping exists depending on weather.

Information on park and campground usage was obtained from Dane County Land & Water Resources, Parks Division (Dane County Parks); the park does not have a written management plan. The year 2018 and 2019 usage data indicated 1,640 camper days in 2018 and 1,793 camper days in 2019 (number of nights that a campsite is occupied). The latest available vehicle counter data for the park's boat launch indicated 70,200 vehicles used the boat launch in 2015.

An archaeological investigation of the park identified a significant pre-contact American Indian habitation and campsite within the project area. Based on the findings of the Phase I study, the site meets the criteria for listing on the NHRP for the potential to contribute to the prehistory of the region. The archaeological site is situated within the park. Because the archaeological site will not be impacted by the US 51 project, Phase II archaeological investigation was not recommended.

Additional information obtained from Dane County Parks includes the following:

- The campground is the only Dane County campground on the Madison chain of lakes with waterfront campsites.
- The revenues from this campground [25 sites total with 30 amperes (amps) electrical to all] are also used to offset maintenance and other costs attributable to other Dane County campgrounds.
- There are a total of 14 campsites along US 51, one of which is special needs accessible and closest to the office and shower building. The 13 standard sites are on a first come, first serve basis.
- Dane County Parks is in favor of replacing the existing chain link fence with a barrier along US 51.
- The loss of mature deciduous shade trees along US 51 would be detrimental to all 14 campsites.
- Dane County Park's concerns include the proximity of US 51 to the office building, loss of landscaping, and loss of setback as a result of the project.
- Because of the archaeological site located within Babcock Park, the number of available sites is maximized at this time and there is no opportunity to relocate campsites to the west.

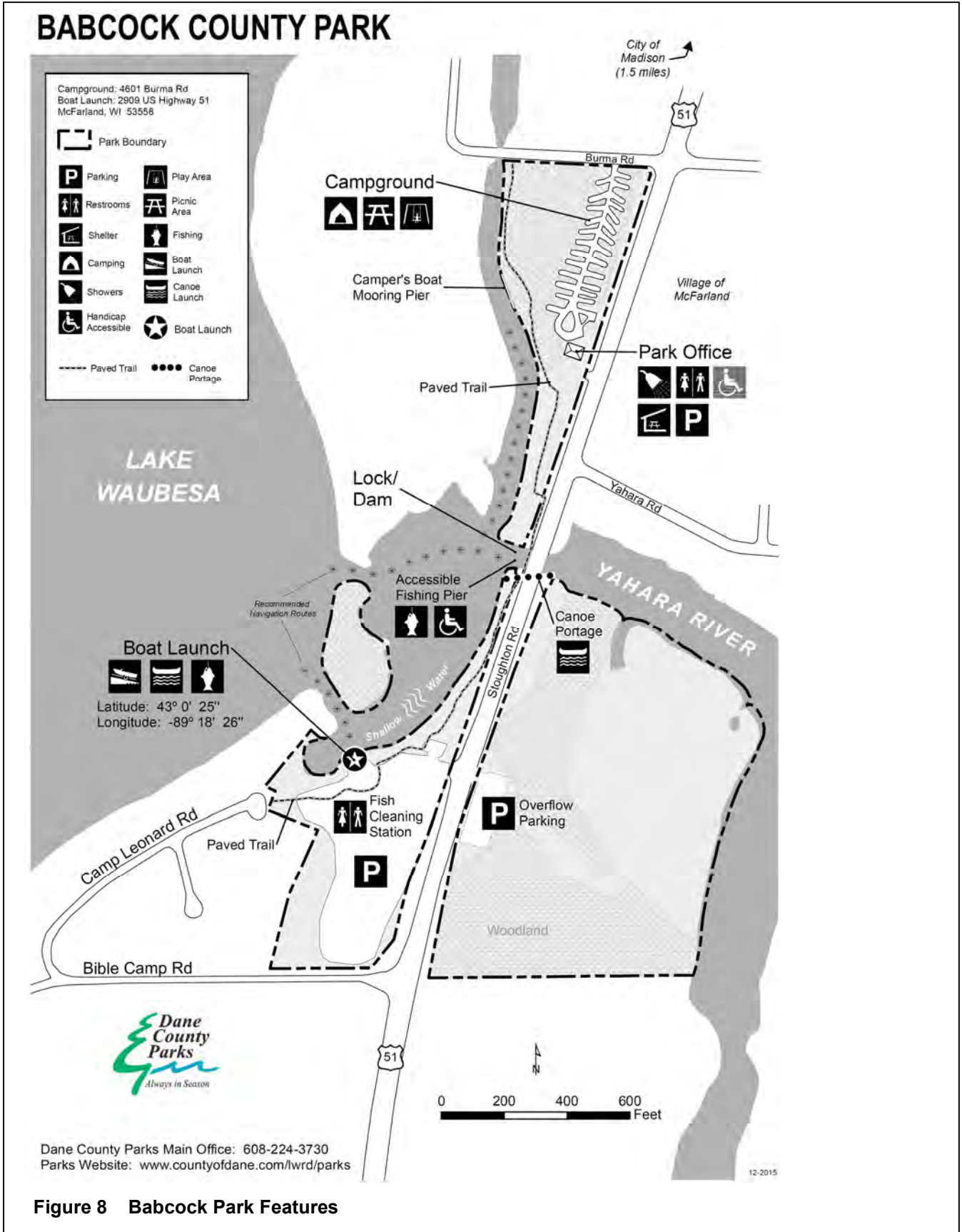


Figure 8 Babcock Park Features

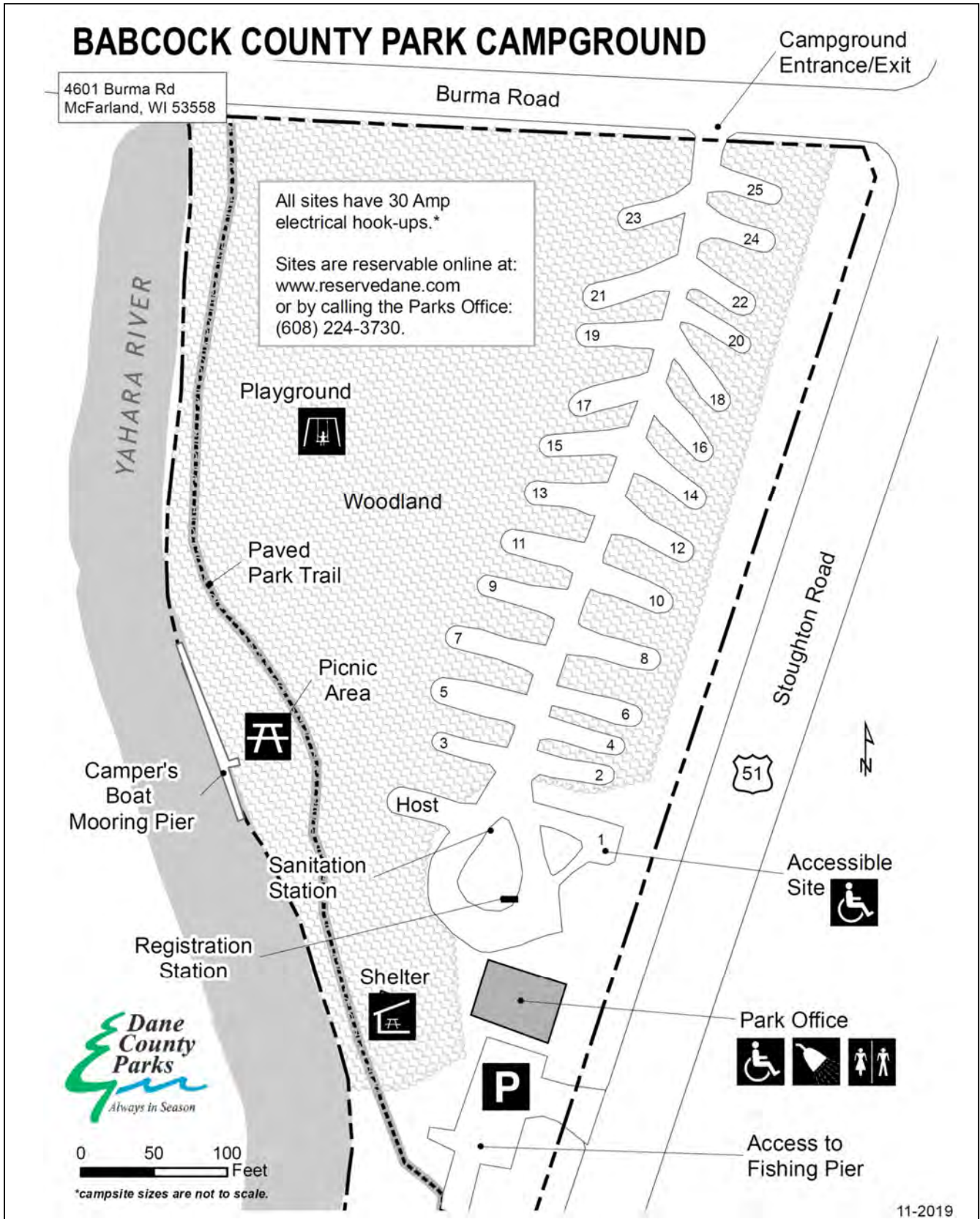


Figure 9 Babcock Park Campground Features

6. Description of Use and Impacts on the Section 4(f) Property

The No Build Alternative would not impact Babcock Park. There would be no use of the Section 4(f) resource and no real estate acquisition.

Alternative A would not impact Babcock Park because the only improvements proposed in the McFarland area are a multilane roundabout at Exchange Street, approximately 925 feet south of the park's south border at Bible Camp Road, and minor safety improvements limited to revising the crosswalk pavement markings. Burma Road is Babcock Park's north boundary (see Figures 7 and 8).

Alternative B and Alternative H are identical for the section from Exchange Street to the project's north terminus at Terminal Drive/Voges Road. Correspondence related to Alternative B for Project ID 5845-06-02 also applies to Alternative H (preferred alternative), evaluated under Project ID 5845-06-03. These alternatives would have identical impacts to Babcock Park. Alternative H is the preferred alternative because it meets an acceptable number of need factors, as well as the project's federal fiscal constraint requirement. Alternative B best meets the project need factors but does not meet the federal fiscal constraint requirement. Impacts cannot be avoided with Alternative H because park land is located on both sides of US 51 and Alternative H widens the roadway in this area and requires real estate acquisition.

Alternative H in the area of Babcock Park includes the following:

- Widen the existing 4-lane roadway (53-foot-wide, back of curb to back of curb) by 6 to 30 feet to a width of approximately 59 to 83 feet (back of curb to back of curb).
- Add a two-way, left-turn lane (TWLTL). The TWLTL would be located between Yahara Drive and Burma Road where an existing 600-foot-long, 16-foot-tall retaining wall runs along the east side of US 51 and the Babcock Park campground is located along the west side of US 51. The TWLTL section provides full access to the northern parking lot for Babcock Park users on the north side of the Yahara River as well as to the park office and shower building. The TWLTL section would be 14 feet wide, the narrowest roadway footprint to minimize R/W impacts to the campground.
- Add right- and left-turn lanes at the Babcock Park boat launch entrance. To improve safety, northbound vehicles leaving the boat launch parking lot would be required to first travel south 0.3 miles to the roundabout at the intersection with Exchange Street before making a u-turn to travel northbound (total of 0.6 miles). A new left-turn lane would be provided for northbound vehicles on US 51 approaching the boat launch entrance.
- Relocate the entrance to the Babcock Park overflow parking lot located on the east side of US 51. The entrance would be shifted approximately 275 feet south of its existing location so that vehicles exiting the overflow lot can travel north on US 51 and access the main boat launch parking lot on the west side of the highway.
- Add a designated left-turn lane at Burma Road, a street with a north entrance to the park campground.
- Provide pedestrian accommodations along both sides of US 51.
- Improve designated pedestrian crossings to provide refuge.

While impacts are minimized, these improvements would require an estimated 0.5 acres of fee R/W or approximately 3.1 percent of the park area in addition to 2.9 acres of temporary limited easement. Figure 10 is a schematic map showing the general locations in Babcock Park where R/W is needed as part of Alternative H. Refer to the Plan Sheets in Appendix B for more detailed maps showing areas of required fee R/W and easement acquisition.

Alternative H would not cause a noise impact to Babcock Park. Criteria used to define traffic noise impacts have been established by WisDOT. Traffic noise impacts occur when the predicted equivalent sound level (Leq) approaches or exceeds the noise level criteria (NLC) established for a type of land use or when predicted sound levels substantially increase above existing levels. For land uses that include parks and recreation areas, the NLC is 67 A-weighted decibels (dBA). WisDOT has determined the predicted equivalent sound level "approach" is defined as 1 dBA less than the NLC and the "substantial increase" is defined as 15 dBA or more than existing levels. The traffic noise analysis for Alternative H determined that no noise receptors in Babcock Park would be exposed to noise levels that approach or exceed the respective NLC.

BABCOCK COUNTY PARK

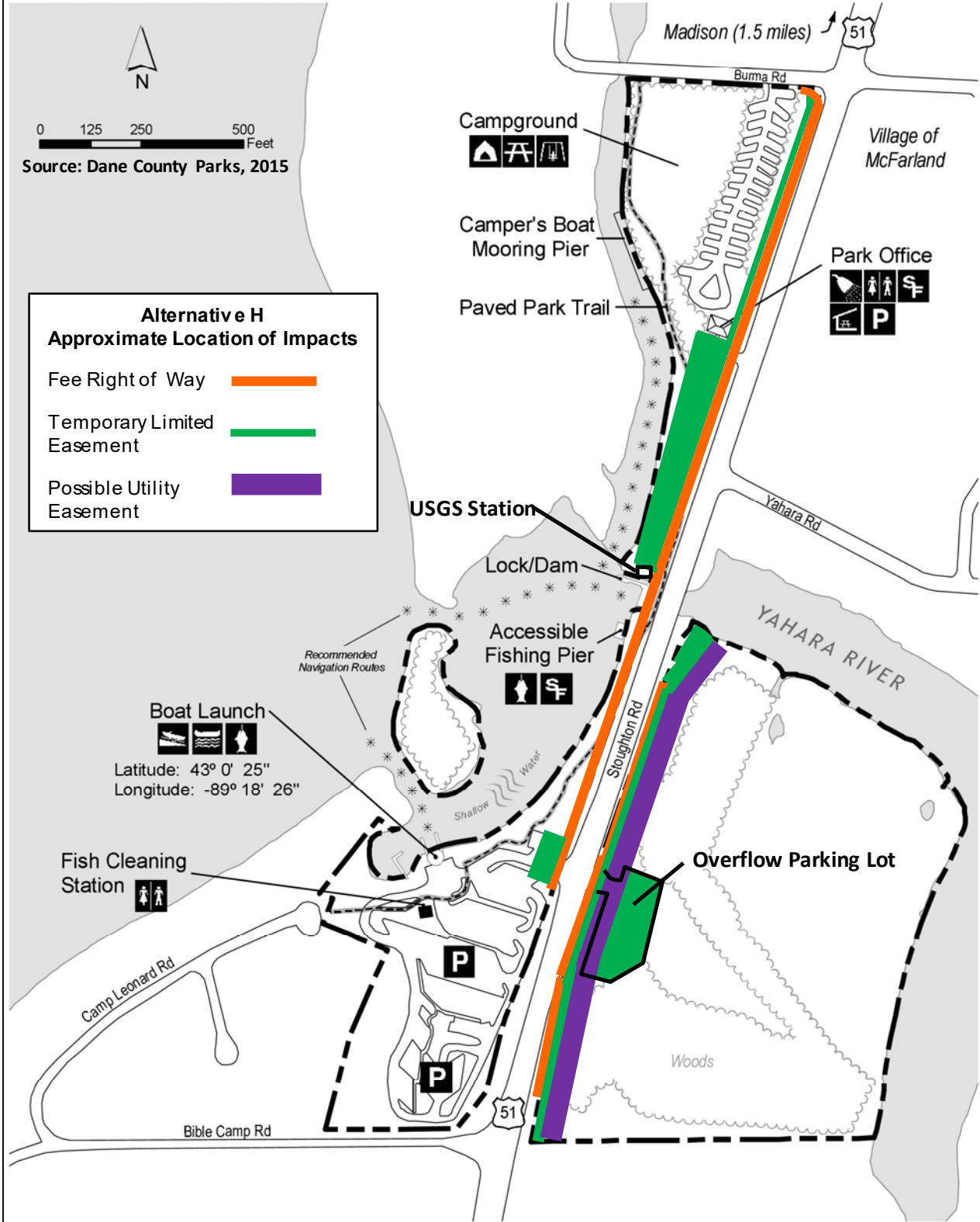


Figure 10 Alternative H R/W Impacts–Babcock Park

Alternative H would also result in an impact to Babcock Park related to a sanitary sewer force main. KSD maintains a sanitary sewer force main in a 20-foot permanent easement that runs along the east side of US 51 within the boundaries of Babcock Park. The widening of US 51 would cause KSD to shift the force main to the east so that it is not located within the newly expanded US 51 R/W. Shifting the force main would likely require KSD to obtain additional permanent easement through Babcock Park. Temporary construction easement may also be needed.

This temporary impact would not be considered a Section 4(f) use because:

- Duration is temporary and there is no change in ownership of the land.
- Scope of work is minor in nature and magnitude of changes to Section 4(f) property is minimal.
- There will be no anticipated permanent adverse physical impacts or interference with the protected activities, features, or attributes of the property on either a temporary or permanent basis.
- The land being used will be fully restored and returned to a condition, which is at least as good as what existed before the project.
- There is documented agreement on the above conditions with officials having jurisdiction over the Section 4(f) resource.

Dane County Parks is aware of the need for easements associated with the force main. Alternative H is the preferred alternative and construction staging of the force main will be coordinated with Dane County Parks. Dane County Parks will be notified of construction impacts and disturbed lands will be restored as soon as construction in the vicinity of the park is completed. The general location of the utility easement is shown on Figure 10. Refer to the Plan Sheets in Appendix B for more detailed maps showing areas of additional easement acquisition.

7. Avoidance Alternatives

No Build Alternative

The No Build Alternative would not include improvements to US 51, and R/W would not be acquired from Babcock Park. The No Build Alternative does not meet the project's purpose or any of the need factors. Although it is feasible, it is not prudent because it does not address safety or operational problems.

Alternative A—Low Build

Alternative A would not include improvements to US 51 adjacent to Babcock Park and as a result no R/W would be acquired from Babcock Park. From an overall project perspective, Alternative A is a feasible avoidance alternative, but it is not prudent because it does not address the project need factors in the McFarland area as well as Alternative H. A comparison of how the build alternatives address the need criteria for the McFarland area, and Babcock Park specifically, is provided in Table 1 in Section 3.

Investigation of Off-Alignment Alternatives

According to 23 CFR 774, feasible and prudent avoidance alternatives must be investigated before impacting park resources. Because Babcock Park is located on both sides of US 51, an off-alignment route would be required to avoid Babcock Park. The alignments considered would route US 51 west of Lake Waubesa or east of Babcock Park through residential neighborhoods in McFarland. See Figure 11 for a graphic of the off-alignment alternatives considered.

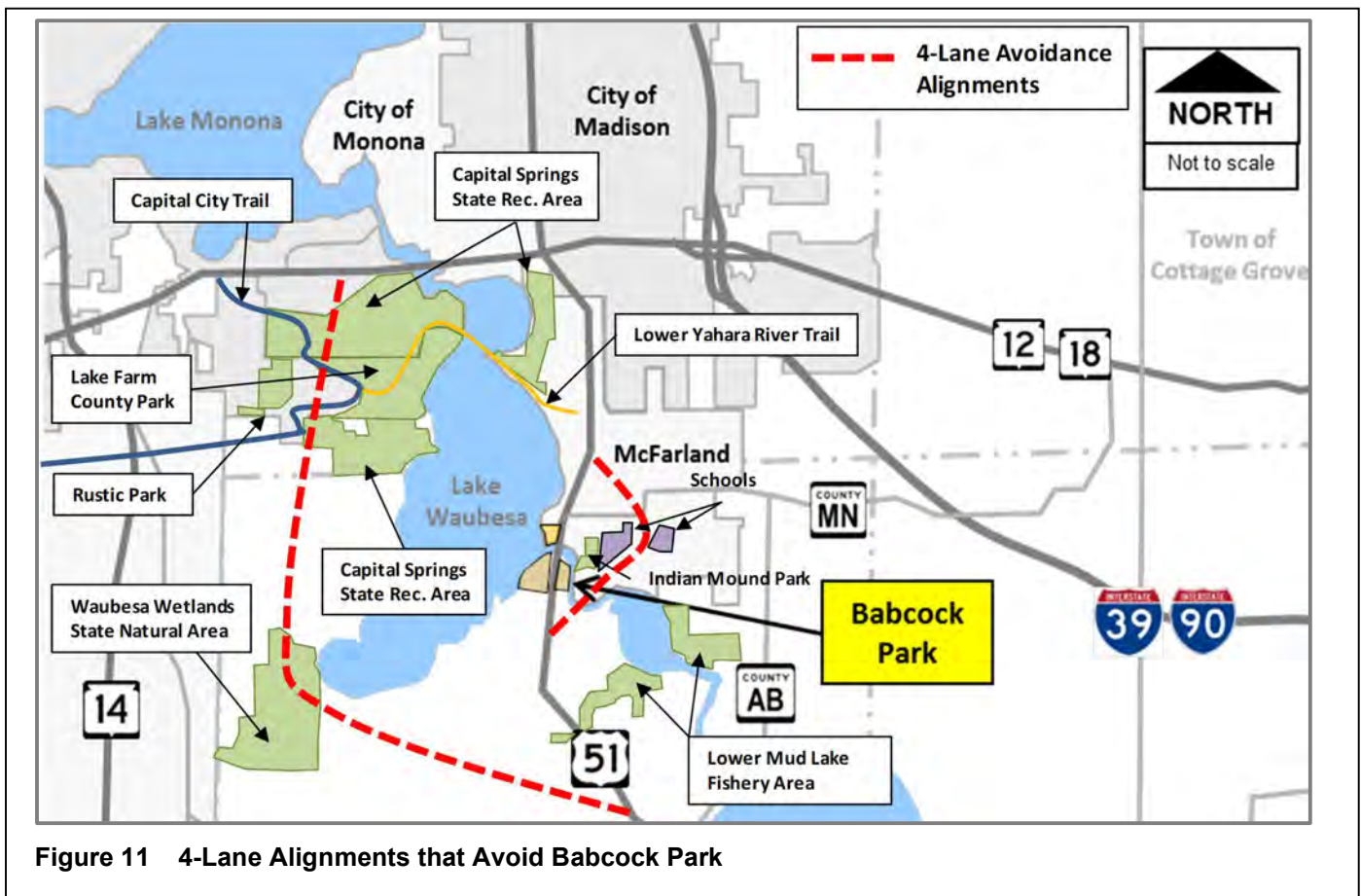
Both of the off-alignment alternatives could feasibly be constructed to avoid Babcock Park, but the alignments would not be prudent. Both off-alignment alternatives would result in impacts to resources other than Babcock Park that are protected by Section 4(f) and cannot be considered avoidance alternatives.

An alignment around the west side of Lake Waubesa would require more than 6 miles of new 4-lane roadway to rejoin US 12/18 near the West Broadway interchange. This alignment would likely have to cross the Waubesa Wetlands State Natural Area located at the southwest end of Lake Waubesa. As the potential alignment proceeded north, it would likely have to cross wetlands and would cross the Capital Springs State Recreation Area and Capital City Trail. It could also potentially impact Lake Farm County Park and Madison Metropolitan Sewerage District ponds. There are also four Madison Parks in the area, including Rustic Park, Indian Springs Park, Baxter Park, and Ocean Road Park. The Capital Springs State Recreation Area,

Lake Farm Park, Rustic Park, Indian Springs Park, Baxter Park, and Ocean Road Park are protected by Section 4(f). This potential alignment could create an additional 2.5 miles of indirection for motorists.

An off-alignment alternative to the east to avoid Babcock Park would need to leave the current alignment of US 51 near Exchange Street and cross the Yahara River. This could require a new bridge at Exchange Street or potentially a new river crossing. The east alignment could impact wetlands and the alignment would impact Legion Memorial Park, Arnold Larson Park, or Indian Mound Conservation Park (listed on the NRHP in 1984), before rejoining the existing US 51 north of Burma Road. These parks are protected by Section 4(f). The hilly topography in this area could also result in impacts to the McFarland High School and Indian Mound Middle School (both of which are finishing up major renovations) located along the east boundary of Indian Mound Conservation Park. This avoidance alignment would likely require residential and business relocations.

An off-alignment alternative shifted even farther east or west to avoid the resources discussed is not prudent and would create additional indirection for motorists. With US 14 located approximately 4 miles to the west and I-39/90 located approximately 3.25 miles to the east, US 51 is needed on or near its current alignment.



While the off-alignment alternatives would avoid Babcock Park, they cannot be considered avoidance alternatives because they would result in extensive impacts to other resources protected by Section 4(f) as well as unreasonable economic and social impacts with severe disruption to the McFarland community.

The avoidance alternatives are the No Build Alternative and Alternative A (Low Build Alternative). The No Build Alternative does not meet the project's purpose or any of the need factors. Although it is feasible, it is not prudent because it does not address safety or operational problems. Alternative A is a feasible avoidance alternative, but it is not prudent because it does not address the project need factors in the McFarland area as well as Alternative H. There is no feasible and prudent avoidance alternative to the use of land from the Section 4 (f) property.

8. All Possible Planning to Minimize Harm

As indicated in Section 7, no feasible and prudent alternative was identified that avoids the Babcock Park Section 4(f) property. Alternative H is the preferred alternative and all possible planning to minimize harm has been incorporated into the alternative. WisDOT and FHWA have incorporated the following measures to minimize harm to Babcock Park.

Alternative H was designed to minimize the amount of new R/W required from Babcock Park in the following ways:

- South of the Yahara River on the west side of US 51, the terrace area between sidewalk and curb was eliminated and a retaining wall is used to avoid impacts to the parking lot.
- North of the river, the terrace area between sidewalk and curb was removed.
- The existing 600-foot retaining wall on the east side of US 51 between Yahara Drive and Burma Road is a design constraint that controlled the roadway section. The use of a TWLTL instead of extending the median reduced the roadway width by 2 feet.
- Slope widths and R/W requirements were reduced by using retaining walls along the west side of the roadway.

R/W impacts were reduced by approximately one acre by minimizing the roadway section “footprint” and using retaining walls. WisDOT and FHWA will continue to refine the US 51 design to further reduce impacts to Babcock Park, if possible.

In June 2013, WisDOT obtained an appraisal report for the campground portion of Babcock Park. The appraisal concluded that considering the mitigation measures as part of Alternative H (which are the same impacts as Alternative B that was being evaluated at that time), the physical and economic impacts on the campsites along US 51 as a result of Alternative H are nominal. The report concluded that the campsites along US 51 and the campground property are of equal utility in a post-Alternative H condition compared to present condition. While the temporary limited easement for construction purposes would have a negative impact on the campsites, it would only be for the duration of construction adjacent to the campground.

9. Measures to Mitigate Harm

WisDOT will compensate Dane County Parks for the acquisition from Babcock Park before the reconstruction of US 51 adjacent to the park. A list of mitigation measures agreed to by Dane County Parks and WisDOT are listed here and shown on Figures 12 through 18, and on the preliminary plan sheets in Appendix B. Park features are shown on Figures 8 and 9. WisDOT will continue to work with Dane County during the final design phase to refine these mitigation measures.

1. WisDOT will include provisions for wayfinding signage to the park, campground, and boat launch for northbound and southbound traffic.
2. WisDOT will replace trees lost within Babcock Park because of construction; location, size, and type of trees will be determined.
3. WisDOT will include relocation and recalibration of the United States Geological Survey (USGS) station at Babcock Park.
4. WisDOT will provide an access path from proposed US 51 path south of the Yahara River bridge to the existing fishing pier and dam.
5. WisDOT will provide a crosswalk on US 51 with pedestrian refuge islands near the overflow parking lot on the east side of US 51.
6. WisDOT will provide a shared-use path from the overflow parking area on the east side of US 51 to the Yahara River.
7. WisDOT will provide sidewalk on the east side of the Yahara River Bridge and a bicycle and pedestrian path on west side of the bridge.
8. WisDOT will provide a connector path from the proposed US 51 path north of the Yahara River bridge to the parking lot and existing park path on the west side of US 51.
9. WisDOT will reconstruct the lock parking lot as single loaded on the west side and expand the lot north to

the existing storage sheds.

10. WisDOT will discuss with Dane County Parks the options for decreasing the entrance drive slope to the shower building parking lot. If needed, the parking lot will be raised and reconstructed with required stormwater facilities.
11. WisDOT will lengthen the span of the Yahara River Bridge to be at least the same as the existing dam structure opening.
12. WisDOT will construct a retaining wall from Station (Sta.) 489+00 to Sta. 494+00 that includes a transition ramp to provide access to the parking lot.
13. If needed to maintain existing boat landing parking lot roadway geometry, WisDOT will provide a retaining wall from approximately Sta. 478+50 to approximately Sta. 481+00.
14. WisDOT will provide a screening and/or barrier wall adjacent to the campground. Between the wall and US 51 west curb line, sidewalk will be provided.
15. WisDOT is willing to provide some aesthetic and informational provisions on the screening and/or barrier wall and will coordinate these items with Dane County Parks.

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BABCOCK PARK MITIGATION MEASURES

1. WisDOT will include provisions for way-finding signage to park, campground, and boat launch for north- and southbound traffic.
2. WisDOT will replace trees lost within Babcock Park because of construction; location, size, and type of trees will be determined.
3. WisDOT will include relocation/recalibration of the USGS station at Babcock Park.
4. WisDOT will provide an access path from proposed US 51 path south of the Yahara River bridge to the existing fishing pier and dam.
5. WisDOT will provide a crosswalk on US 51 with pedestrian refuge islands near the overflow parking lot on the east side of US 51.
6. WisDOT will provide a shared use path from the overflow parking area on the east side of US 51 to the Yahara River.
7. WisDOT will provide sidewalk on the east side of the Yahara River Bridge and a bicycle/pedestrian path on west side of the bridge.
8. WisDOT will provide a connector path from the proposed US 51 path north of the Yahara River bridge to the parking lot and existing park path on the west side of US 51.
9. WisDOT will reconstruct the lock parking lot as single loaded on the west side and expand the lot north to the existing storage sheds.
10. WisDOT will discuss with Dane County Parks the options for decreasing the entrance drive slope to the shower building parking lot. If needed, the parking lot will be raised and reconstructed with required stormwater facilities.
11. WisDOT will lengthen the span of the Yahara River Bridge to be at least the same as the existing dam structure opening.
12. WisDOT will construct a retaining wall from Station 489+00 to Station 494+00 that includes a transition ramp to provide access to the parking lot.
13. If needed to maintain existing boat landing parking lot roadway geometry, WisDOT will provide a retaining wall from about Station 478+50 to about Station 481+00.
14. WisDOT will provide a screening and/or barrier wall adjacent to the campground. Between the wall and US 51 west curb line, sidewalk will be provided.
15. WisDOT is willing to provide some aesthetic and informational provisions on the screening and/or barrier wall and will coordinate these items with Dane County Parks.



Figure 12 Impacts and Mitigation Measures at Babcock Park



Figure 13 Distance from Boat Launch Parking Lot to Exchange Street Roundabout

Considering the mitigation measures listed previously and shown on the attached mapping, use of the Section 4(f) resource will not adversely impact the activities, features, and attributes in the following ways:

Relocation and Recalibration of USGS Station (Mitigation Item 3)

The existing USGS station is located on the north side of the Yahara River, near the lock and dam control structure. In this area, a strip of R/W will be acquired from the park for the new bridge and multiuse path. The lock and dam parking lot will be reconstructed and expanded to the north and the USGS station will be relocated to the west, near the lock and dam control structure (see Figure 14).

Shore Fishing (Mitigation Items 4, 5, and 6)

Shore fishing areas and accessible fishing platforms will not be directly impacted. Improvements to park paths and sidewalks will improve access to designated shore fishing locations as well as other shoreline areas in the park. On the west side of US 51, access paths will be reconstructed south of the Yahara River bridge to the existing fishing pier and dam shore fishing area (see Figure 14).

Along the east side of US 51, a new path will extend from the overflow parking lot to the Yahara River, providing a new, accessible shore fishing location and canoe launch location. Canoe launching facilities will be improved by the addition of this path. Also, canoeists that park in the overflow lot and wish to launch a canoe on the west side of US 51 will be able to cross safely at a new pedestrian crossing that will be provided near the overflow parking lot (see Figure 15).

Yahara River Bridge (Mitigation Item 7)

Sidewalk will be added to the east side of the new Yahara River Bridge and a multiuse path will be provided on the west side of the bridge (see Figure 14).

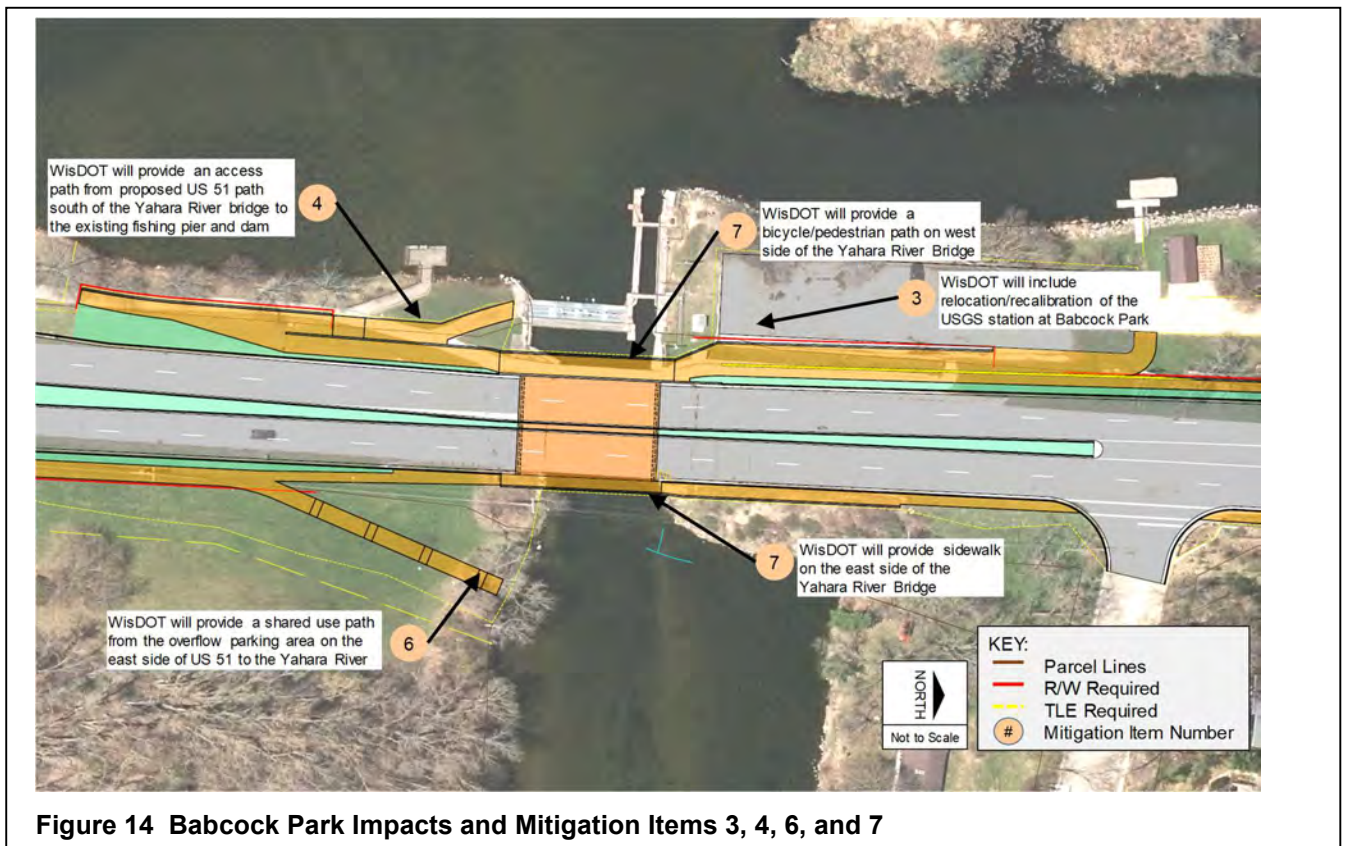
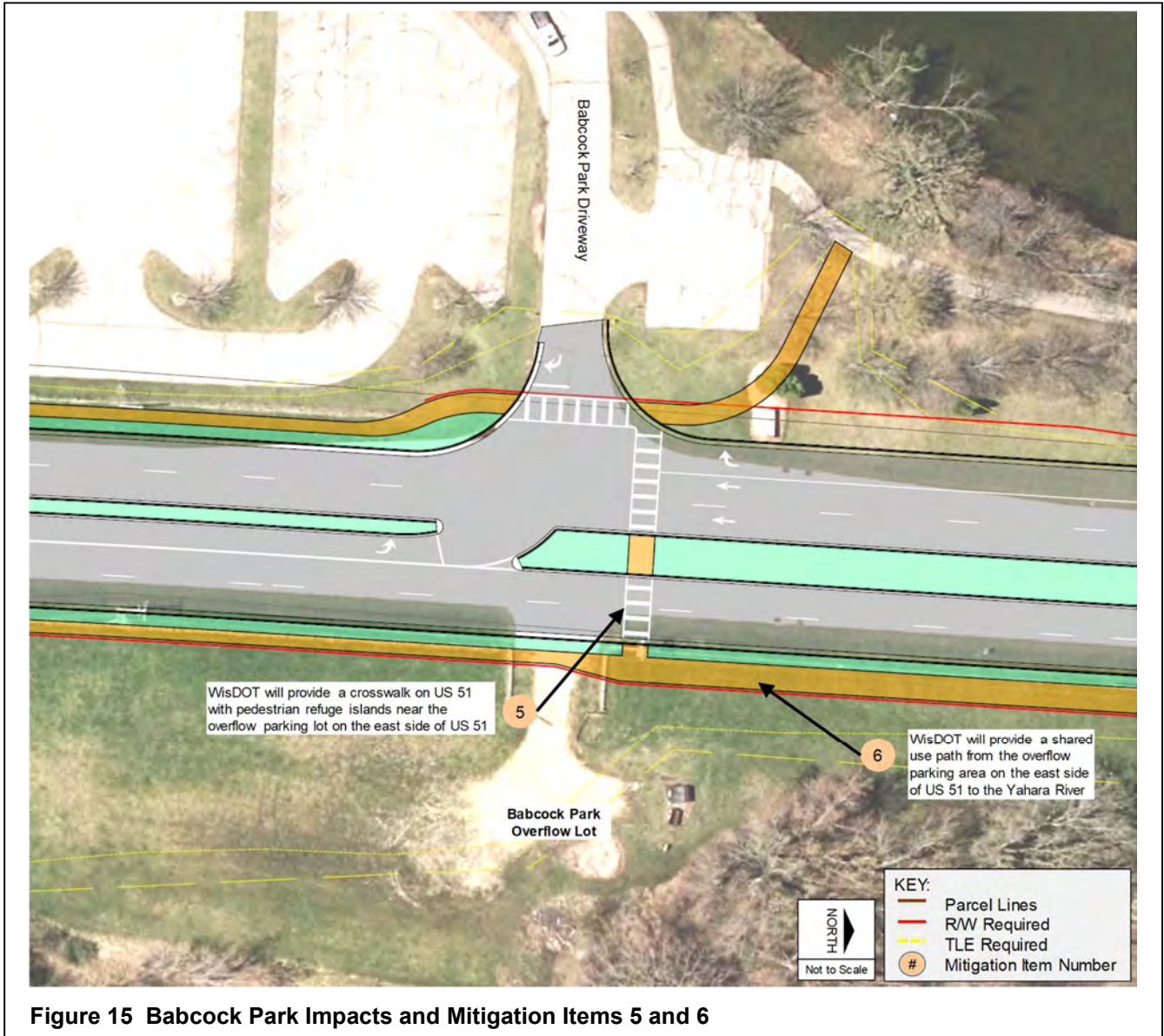


Figure 14 Babcock Park Impacts and Mitigation Items 3, 4, 6, and 7



Connector Path and Lock Parking Lot Reconstruction (Mitigation Items 8 and 9)

On the west side of US 51, north of the Yahara River, a new connector path will be constructed from the proposed US 51 path to the parking lot and existing park path. The lock parking lot will be reconstructed as single loaded on the west side and will be expanded north to the existing storage sheds (see Figure 16).

Lengthen Span of Bridge (Mitigation Item 11)

The span of the Yahara River Bridge will be lengthened to be at least the same as the existing dam structure opening (see Figure 16).

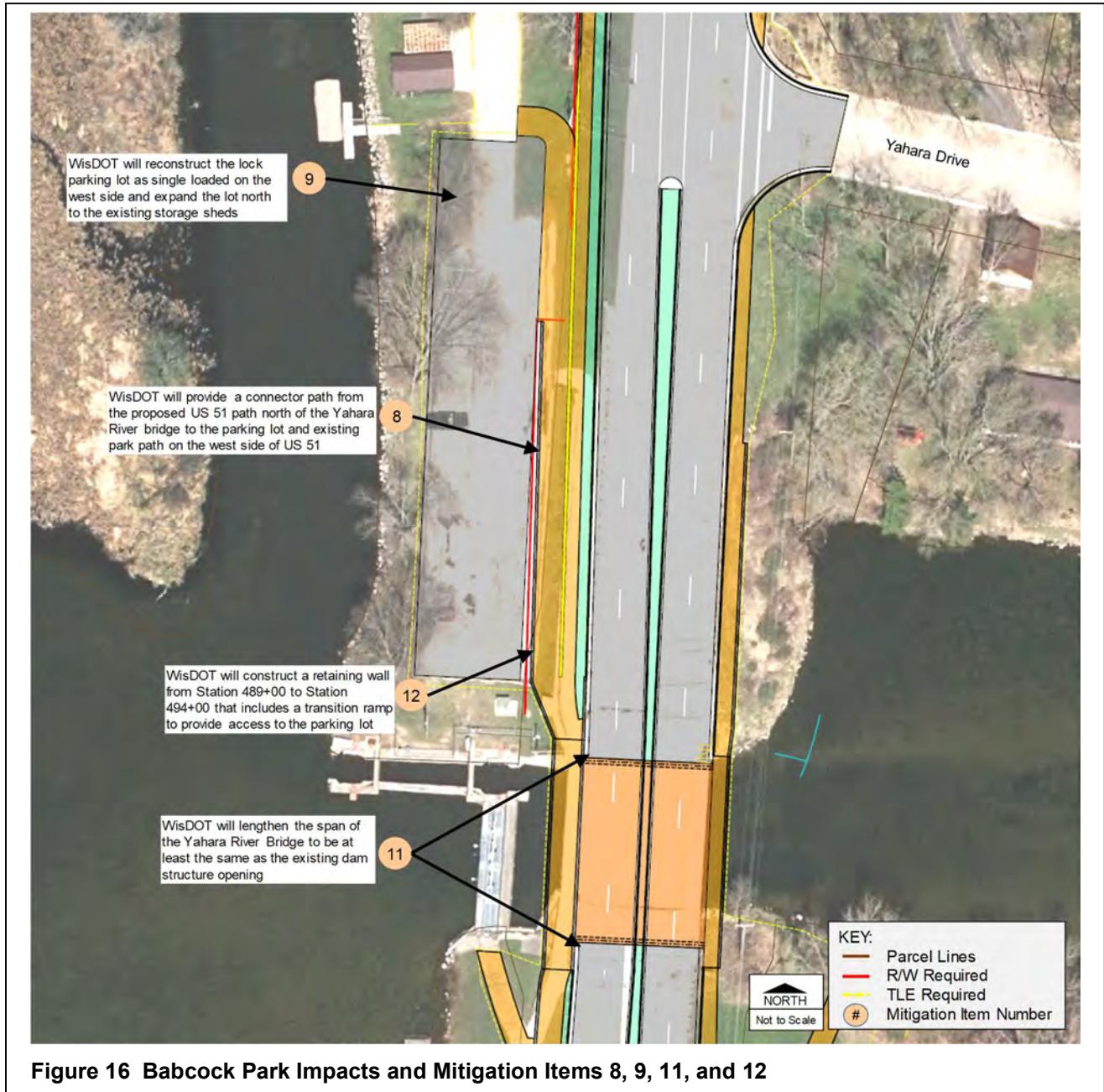
Retaining Wall and Transition Ramp (Mitigation Item 12)

An approximately 500-foot-long retaining wall will be constructed from the north end of the Yahara River bridge and will include a transition ramp to provide access to the lock parking lot and the existing park path (see Figure 16).

Boat Launch Parking Lot (Mitigation Item 13)

If needed, a retaining wall will be constructed to minimize highway impacts to the boat launch parking lot so that no parking spaces will be impacted. Access to the boat launch facility will be improved by the additional turning lanes. A

safer exit from the boat launch facility to travel north on US 51 is proposed with a right-out turning movement and a U-turn at the proposed roundabout at Exchange Street approximately 1,650 feet south of the entrance (Figure 13).



Camping and Picnicking (Mitigation Item 14)

A retaining wall will be provided to reduce fill slopes adjacent to the campground. A barrier wall or retaining wall will be used to provide a visual screening of US 51 for Babcock Park users. The height of the screening wall will be determined in consultation with Dane County Parks. The wall could extend from Burma Road to the Babcock Park shower building.

Between the wall and US 51 curb line, a crash barrier and sidewalk will be provided. The crash barrier will protect the campers from errant vehicles (see Figure 17). Wall design details are being discussed with Dane County Parks. The distance between the nearest campsite parking pads and the retaining/screening wall ranges from approximately 34 to 42 feet. See Figure 18 showing the distances from the screening wall to various campsite parking pads.

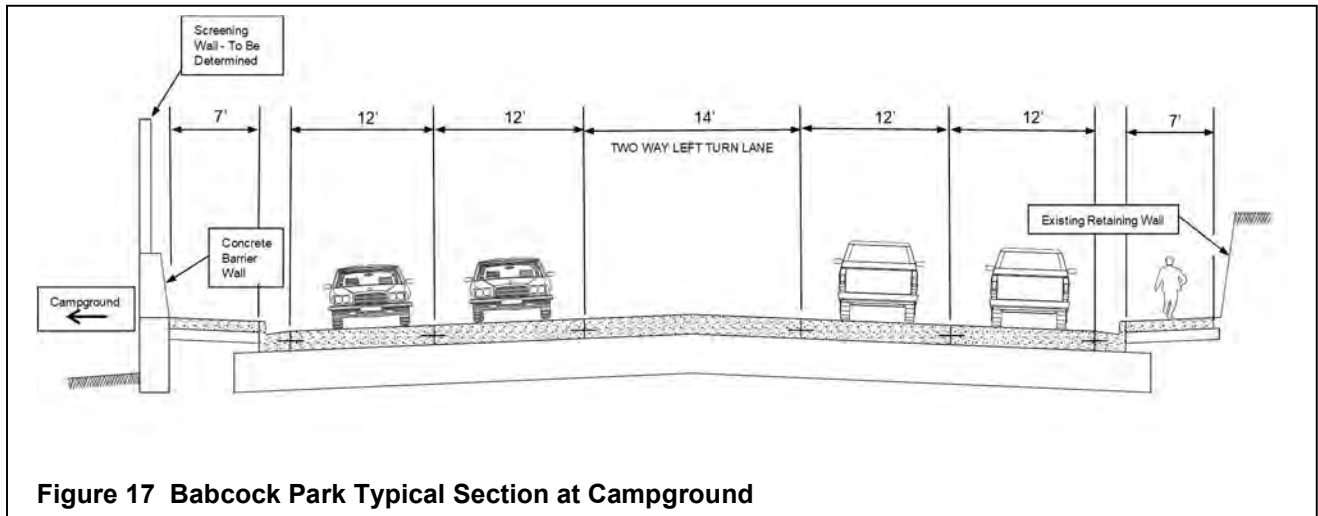


Figure 17 Babcock Park Typical Section at Campground

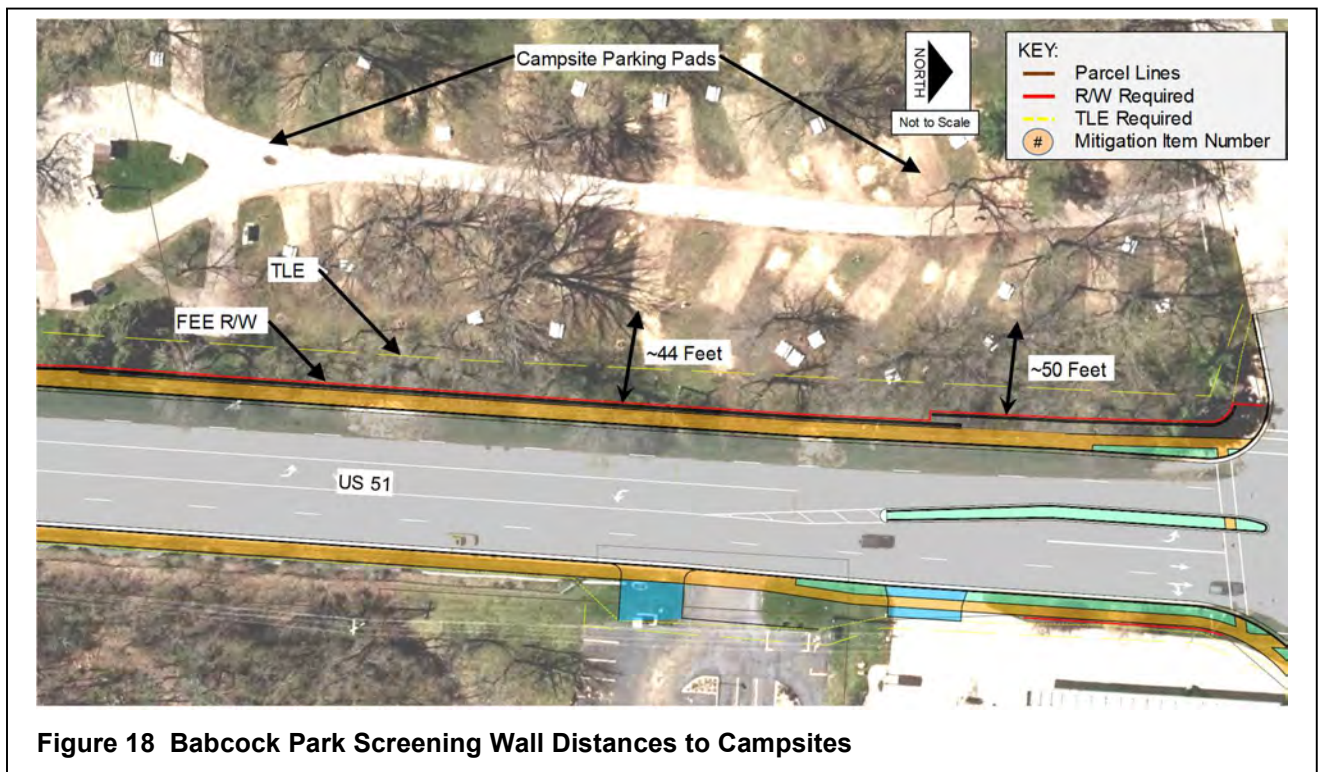


Figure 18 Babcock Park Screening Wall Distances to Campsites

10. Coordination

WisDOT and FHWA met with Dane County Parks and the Park Commission on several occasions to discuss the potential impacts to Babcock Park and proposed mitigation measures. Dane County Parks initially proposed 18 mitigation measures (August 24, 2011) and WisDOT agreed to satisfy 15 of those measures (October 14, 2011). The following three mitigation measures were not possible: (1) WisDOT is unable to begin any improvements within a five-year time frame; (2) WisDOT is unable to install a pedestrian underpass at the Yahara River bridge because of grade considerations; and (3) WisDOT is unable to fund a campground at an off-site location because WisDOT is not able to provide money as a mitigation measure. In February 2013, Dane County Parks requested that some of the existing campsites be relocated within the park. The relocation of the existing campsites will not be considered a

mitigation measure because the proposed location for relocating the campsites was identified as an archaeological site.

Public involvement for the US 51 Corridor Study has been ongoing since 2005 when the initial Alternatives Solutions Workshop was held following the Needs Assessment. Following that workshop, Public Involvement Meetings (PIMs) were held in 2006, 2009, 2011, and 2012. These PIMs presented the No Build Alternative, Alternative A–Low Build, and Alternative B–Four-lane Expansion.

The following comments at PIMs in 2011 and 2012 were related to Alternative B:

- At the April 2011 PIM, one comment was provided that requested a connection to Babcock Park from Exchange Street.
- At the October 2012 PIM, there were three residents that supported the impacts to Babcock Park and five residents that opposed the impacts to Babcock Park.

The most recent PIMs for the study were held in 2015, 2019, and October 2020. The alternatives considered in the EA and presented at the meetings were the No Build Alternative, Alternative A–Low Build, Alternative B–Four-lane Expansion, and the build alternative developed for the EA (Alternative H). Updated alternatives and impacts, including impacts to the Babcock Park Section 4(f) property, were presented at the PIMs. Based on a comment received at the 2015 PIM, WisDOT coordinated with Dane County Parks and shifted the overflow parking lot entrance approximately 275 feet south. The shifted driveway is shown on Figures 12 and 13 and the Preliminary Plan Sheets in Appendix B.

A public hearing was held for the study in April 2021. No comments or testimony were provided related to the preferred alternative's impacts to Section 4(f) properties.

Table 4 Correspondence with Dane County Parks	
Date	Correspondence Topic and Meeting Topics/Issues Resolved
9/5/2008	Meeting with Dane County Parks to review alignments and typical sections and discuss options for sidewalks and paths at Babcock Park.
11/10/2008	Dane County Parks email to WisDOT accepting invitation to become a participating agency.
9/7/2010	Meeting with Dane County Parks to discuss potential project impacts to Babcock Park and intersection improvements.
5/13/2011	Meeting with Dane County Parks to discuss potential project impacts at Babcock Park, potential mitigation measures, and design refinements.
7/13/2011	WisDOT attended the Park Commission meeting and presented an overview of the US 51 project and summary of preliminary impacts to Dane County's Babcock Park and potential mitigation measures.
8/24/2011	Letter to WisDOT proposing 18 mitigation measures at Babcock Park.
10/14/2011	WisDOT letter to Dane County Parks responding to proposed 18 mitigation measures at Babcock Park. WisDOT agreed to all requests by Dane County Parks except for the following three: (1) WisDOT is unable to commit to beginning any improvements within a five-year time frame; (2) WisDOT is unable to install a pedestrian underpass at the Yahara River bridge because of grade considerations; and (3) WisDOT is unable to fund a campground at an off-site location because WisDOT is not able to provide money as a mitigation measure.
10/31/2011	Meeting with Dane County Parks to discuss potential project impacts at Babcock Park and WisDOT proposed mitigation measures.
11/28/2011	Letter from Dane County Parks to WisDOT indicating the Park Commission was generally in agreement with the 15 proposed mitigation measures at Babcock Park.
1/17/2013	Meeting with Dane County Parks to discuss Babcock mitigation measures and whether WisDOT should pursue a <i>de minimis</i> impact finding at Babcock Park or a full Section 4(f) Evaluation.
2/27/2013	WisDOT attended the Park Commission meeting to discuss Babcock Park mitigation measures and whether WisDOT should pursue a <i>de minimis</i> impact finding at Babcock Park or a full Section 4(f) Evaluation. There was a motion by the Park Commission reconfirming the Park Commission's position that there is not a <i>de minimis</i> impact to the park unless all 15 mitigation measures are provided and the campsites are relocated. Therefore, if an alternative impacting the park will be implemented, WisDOT will pursue a full Section 4(f) finding for Babcock Park.
10/13/2015 and 10/28/2015	Email correspondence indicating Dane County Parks is in agreement with moving the overflow lot entrance 250 feet south and grading the lot with a 20:1 slope.
8/12/2019	Meeting with Dane County Parks to discuss updated project impacts at Babcock Park and WisDOT proposed mitigation measures.
11/11/2019	Email correspondence providing Dane County Parks concurrence with the need to temporarily detour the trail during construction of the US 51 bridges over the trail.
11/25/20	Meeting with Dane County Parks to provide a status update on the study, discuss the next steps moving forward and the impacts and mitigation items at Babcock Park.

Representatives from Dane County Parks participated in Technical Advisory Committee (TAC) meetings and several agency meetings.

Table 5 Dane County Parks Coordination

Meeting Dates	Group	Topic
6/9/2005	TAC	Reviewed findings from Alternative Solutions Workshop and discussed potential screening of alternatives that would improve other corridors besides US 51 to address US 51 concerns.
8/30/2005	TAC	Presentation of screening results of various improvement alternatives and discussion of implications of other alternatives.
2/20/2006	TAC	Discussed recent meetings, revised Purpose and Need, discussed concept alternatives, suggested refinements and a subalternative of US 51 located west of Stoughton, and discussed upcoming PIM.
9/14/2006	TAC	Provided summary of PIMs, discussed the potential to reduce the number of concepts (result: no), and discussed next steps in agency coordination process.
9/26/2007	TAC	Discussed scope for EIS with overview of requirements of 23 USC 139; reviewed alternatives, typical sections and design criteria, EIS schedule, and public involvement.
3/6/2008	TAC	Discussed project schedule and 23 USC 139 status; reviewed Value Engineering Study goals and recommendations; discussed alternatives to add or remove from EIS and next PIM.
1/20/2009	TAC	Discussed project alternatives and alignments being carried forward, alternatives dismissed, preliminary impacts, traffic modeling results, upcoming agency meeting, upcoming PIMs, and project schedule.
2/19/2009	Agency Meeting (NEPA 404 /Coordination Pt 2)	Reviewed final Purpose and Need, proposed alternatives, and preliminary impacts.
4/28/2009	TAC	Reviewed PIM exhibits and presentation.
6/29/2009	TAC	At this post-PIM meeting, the May 2009 PIM comments were reviewed and refinement and revision of alternatives was discussed.
1/11/2010	Agency Meeting	Provided agencies with a post-PIM project update following May 2009 PIM. Discussed design revisions and new "Stoughton Bypass" alternatives.
1/20/2010	TAC	Reviewed Stoughton Bypass Alternatives, proposed north interchange at County B/AB, and roundabouts at County B (east).
2/8/2011	TAC	Reviewed US 51 design changes and new Stoughton Bypass alternative developed; discussed dismissal of Alternatives C and D.
8/25/2011	Agency Meeting with Field Review	Provided agencies with a post-PIM project update following the April 2011 PIM. Discussed design revisions and potential dismissal of some "Stoughton Bypass" alignments. Meeting included a project corridor field review.
9/25/2012	TAC	Reviewed the US 51 and Stoughton Bypass design changes. Reviewed exhibits for upcoming PIM.

Copies of correspondence between WisDOT and Dane County Parks and copies of meeting minutes related to impacts and mitigation at Babcock Park (when available) are provided in Appendix A.

11. Section 4(f) Finding

Based on the above considerations, FHWA has determined there is no feasible and prudent avoidance alternative to the use of land from the Section 4(f) property. Furthermore, the preferred alternative includes all possible planning to minimize harm to the Section 4(f) resource resulting from such use.

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**APPENDIX A
CORRESPONDENCE**

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List of Correspondence in Appendix A

Date	Page Number	Correspondence or Meeting Topics/Issues Resolved
Babcock Park		
9/5/2008	A-2	Meeting with Dane County Parks to review alignments and typical sections and discuss options for sidewalks and paths at Babcock Park.
11/10/2008	A-4	Email to WisDOT accepting invitation to become a participating agency.
9/7/2010	A-5	Meeting with Dane County Parks to discuss potential project impacts to Babcock Park and intersection improvements.
5/13/2011	A-11	Meeting with Dane County Parks to discuss potential project impacts at Babcock Park, potential mitigation measures, and design refinements.
8/24/2011	A-16	Letter to WisDOT proposing 18 mitigation measures at Babcock Park.
10/14/2011	A-18	WisDOT letter to Dane County Parks responding to proposed 18 mitigation measures at Babcock Park. WisDOT agreed to all requests by Dane County Parks except for the following three: (1) WisDOT is unable to commit to beginning any improvements within a five-year time frame; (2) WisDOT is unable to install a pedestrian underpass at the Yahara River bridge because of grade considerations; and (3) WisDOT is unable to fund a campground at an off-site location because WisDOT is not able to provide money as a mitigation measure.
10/31/2011	A-21	Meeting with Dane County Parks to discuss potential project impacts at Babcock Park and WisDOT proposed mitigation measures.
11/28/2011	A-24	Letter to WisDOT indicating the Park Commission was generally in agreement with the 15 proposed mitigation measures at Babcock Park.
2/27/2013	A-25	WisDOT attended the Park Commission meeting to discuss Babcock Park mitigation measures and whether WisDOT should pursue a <i>de minimis</i> impact finding at Babcock Park or a full Section 4(f) Evaluation. There was a motion by the Park Commission reconfirming the Park Commission's position that there is not a <i>de minimis</i> impact to the park unless all 15 mitigation measures are provided and the campsites are relocated. Therefore, if an alternative impacting the park will be implemented, WisDOT will pursue a full Section 4(f) finding for Babcock Park.
10/13/2015 and 10/28/2015	A-27	Email correspondence indicating Dane County Parks is in agreement with moving the overflow parking lot entrance 250 feet south and grading the lot with a 20:1 slope.
8/12/2019	A-30	Meeting with Dane County Parks to discuss updated project impacts at Babcock Park and WisDOT proposed mitigation measures.
Lower Yahara River Trail		
11/11/19	A-32	Email correspondence providing Dane County Parks concurrence with the need to temporarily detour the trail during construction of the US 51 bridges over the trail.
Brost Addition		
5/14/21 and 5/19/21	A-35	Letters from WDNR and Groundswell Conservancy providing concurrence with the <i>de minimis</i> impact finding.
Section 4(f) Evaluation		
7/1/20	A-39	Letter from US Department of Interior providing concurrence with draft determination and having no objection to the Draft 4(f) Evaluation.



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WisDOT SW Region Office

September 5, 2008, 9:00 a.m.

Participants: Jeff Berens, Tom Koprowski (WisDOT)
Joan Petersen, Pete Wood (Strand)
Darren Marsh, Chris James, Kevin Connors (Dane County)

Meeting Exhibits: Current typical sections
Aerials with alternatives on US 51
Aerial maps with alternatives for bike accommodation in McFarland

Action items: Noted in **bold**

From 8:30 to 9:00 a.m., Strand and WisDOT discussed the latest versions of alternatives with exhibits showing the typical sections and alignments being considered.

The typical sections presented included the Stoughton urban area using a 4-lane, 40 mph low speed urban section and a 4-lane 40 mph+ high speed urban section. Bike accommodation is provided on-street only in these typical sections and sidewalk is provided on the east side of the road with provision for future sidewalk on the west side where curb and gutter will be provided.

The alignments shown were of the Stoughton urban area, the McFarland urban area and the rural section between the two communities. The rural exhibits showed one alignment generally following the existing alignment with an off alignment (westerly) section to avoid an Indian mound south of CTH B/AB and another with an off alignment section (westerly) from S. Brooklyn Road and then turning north to run along the west side of the UW's Synchrotron facility on Schneider Road to match in at US 51 north of Dyreson Road. Also shown was a re-alignment of CTH B west to connect with USH 51 north of the existing location with a flatter profile.

At 9:00 a.m. a meeting commenced with Dane County Parks, Planning and Land Conservation representatives. Among their concerns were:

1. Hydraulics of the existing Babcock Dam. Hydraulic studies have determined that the existing dam is a restriction to the flow on the Yahara River between the lakes. There may be a future proposal to lengthen the bridge just downstream of the dam.
2. Separation of the camping area along USH 51 from the roadway. Currently tent camping takes place near the existing USH 51 roadway and campers are separated from errant vehicles by a chain line fence. Dane County would like to see a barrier wall constructed in this area to control errant vehicles.
3. Provision for the use of Bible Camp Road for users of the boat ramp. Currently all the traffic using the boat ramp uses the driveway on US 51 north of Bible Camp Road. Local residents are concerned with the volume of traffic that would use Bible Camp



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Road if the expanded parking lot had direct access to Bible Camp Road. The expanded lot currently only connects to the initial parking area and access drive.

- a. Chris James will provide Strand with the County's mailing list of Bible Camp Road residents. (done)**
 - b. Strand will review the Bible Camp intersection access issues and add a discussion to the US 51 Concept Plan Report being performed under a work order.**
4. Dane County was shown the alternatives being developed to provide a pedestrian/bike path along the west side of USH 51 from Larson Beach Road to Exchange Street. Two alternatives were discussed, one with just a path added and a second providing a widened roadway section to permit a two way left turn lane in that area. The typical section proposed in the south end of McFarland did not include a two way left turn lane over the bridge. The alternatives shown only required an approximate 7 foot widening (to the west) of the existing bridge on USH 51 over the Yahara River. Dane County representatives were in favor of the multiuse path and asked that a vehicular barrier system be provided to protect campers. Jeff Berens noted that WisDOT would consider the request as part of the alternative development, but that FHWA would need to be consulted on each of the configurations to determine what was acceptable.
5. Dane County has bridge plans to provide a catwalk adjacent to the south abutment of the USH 51 bridge over the Yahara River to be used for portaging canoe traffic. Dane County applied for a grant to construct the project, but the project was not approved.
6. Provision for bike/pedestrian traffic along CTH B north of Stoughton. Dane County noted that in order to match their plans, modifications to the bridge near CTH N will be required as well as provision for a path along CTH B. Crossings under the bridge and near the railroad corridor would be needed. Strand was provided a copy of the Dane County Parks & Open Space Plan 2006-2011 that shows the proposed Dane County bike path system. (the map is also on the Dane County Web site)

The meeting ended at approximately 10:15 a.m.

Jeff,

In response to your letter of October 10, 2008, Dane County accepts the invitation to be a participating agency for Project ID 5845-06-02
USH 51 Corridor Study
I-39/90 to US 12/18 (Madison South Beltline)
Dane County, Wisconsin

Other Dane County Departments, besides Dane County Highway, that anticipate involvement with this process include (but not limited to)

Dane County Planning, Pam Andros, 261-9780, andros@co.dane.wi.us
Land and Water Resources Department, Kevin Connors, 224-3731, connors.kevin@co.dane.wi.us

Please forward the Draft EIS and the Impact Analysis Methodology to them for their review.

Please feel free to contact me with any questions.

Pam Dunphy, PE
Assistant Commissioner
Department of Public Works, Highway and Transportation
Phone: (608)266-4036
Mobile: (608)575-2244
Fax: (608)266-4269
email: dunphy@co.dane.wi.us



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Dane County

Project Meeting with Dane County Parks and Village of McFarland

Minutes

September 7, 2010, at 1 P.M.
WisDOT SW Region Office

Participants:

Jeff Berens, WisDOT Project Manager

Shar Tebeest, WisDOT C.O. Environmental

Mike Hoelker, WisDOT Planning Supervisor

Allan Coville, Village of McFarland

Don Peterson, Village of McFarland

Luke Hellermann, Strand Associates

Jenny Grimes, WisDOT Environmental

Tom Koprowski, WisDOT Planner

Mike Harried, McFarland Village President

Darren Marsh, Dane County Parks Director

Chris James, Dane County Parks Planner

Joan Petersen, Strand Associates

Exhibits:

Typical sections, Plan/Profile and Cross Sections (Exchange Street to Larson Beach Road, McFarland)

Roll plot - US 51, Tower Road to Larson Beach Road, McFarland

Babcock Park map

Impacts to Babcock Park, memo

Section 4(f) and de minimus impacts PowerPoint handout

Right-In Right-Out Left-In plan view for typical driveway

Viking Park plan sheets (two)

Roll plots, County B and alternatives east of County N to Pleasant Hill Road

Action items: Noted in **bold**; a summary list is provided on the last page of minutes

1. Introductions.

Jeff welcomed the group and indicated the purpose of the meeting was to provide a project update and review the current designs in McFarland and at Dane County's Babcock and Viking Parks.

2. Current US 51 design in McFarland.

Joan summarized the current urban and rural typical sections from East Tower Road, through the Exchange Street roundabout (RAB), and past Babcock Park in McFarland. This included a summary of posted speed limits, sidewalk, bike lanes, and median width. Joan noted the design presented was the same as that at the May 2009 public information meetings (PIMs).

Allan expressed concern with snow removal on the proposed sidewalks adjacent to Babcock Park and the existing retaining wall. Without a terrace there would be no room for snow storage. Joan Petersen agreed that the snow would need to be removed from the sidewalks.

Don asked why sidewalk is proposed only on the east side of US 51 from the Exchange Street RAB to the Yahara River. Don indicated the park is a destination and a sidewalk on the west side might be used more. A sidewalk on the east side might promote pedestrian crossing of US 51 at various locations, where crossing provisions are not located, by those who want to access the park. Joan indicated the project team can look at this closer and consider sidewalk on both sides of US 51. FHWA requires projects to provide for bike and pedestrian accommodations in urban areas.

Don pointed out that there is a crest of a hill just south of the proposed Exchange Street RAB and that this may be a safety issue for northbound vehicles approaching the RAB. Joan agreed and indicated that the hill would probably need to be lowered for proper sight distance for motorists approaching the RAB from the south. Joan also pointed out that speeds would be reduced approaching the RAB with typical speeds through the RAB of about 17 to 20 mph.



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Darren confirmed the park's overflow parking area on the east side of US 51 is used during busy weekends. The area is also used to store the park's dumpsters. However, Parks (Parks) would be willing to consider removal of the access to that area if it would be helpful.

The potential "cat walk" crossing for canoeists under the existing bridge was also discussed. Parks would like to see it constructed but was under the impression the funding is not currently available. Tom thought the funding may have been allocated.

- **Tom will check on the funding status.**

3. Joan initiated a discussion of existing park access to US 51 at STA 493+00.

- a. At the access to the park office (known as the Lock Access), in addition to the strip right-of-way (R/W) acquisition (about 8 feet), temporary limited easement (about 10 to 15 feet) for re-construction of the access would be needed. Plans propose an approximate 8 percent slope and based on Joan's observations at the site, it appears that would be an acceptable modification. The question was raised as to whether or not the 8 percent slope was too much for boats/trailers and RVs. Darren indicated that boats and RVs are not supposed to use that access point and that the addition of signage to indicate the restriction could be a potential mitigation measure.
- b. Chris asked about a connection from the boat ramp parking lot to Bible Camp Road and whether the current US 51 access to the boat ramp lot would remain. Joan confirmed that the existing plans are for the existing access to remain and that there are no plans to connect the lot to Bible Camp Road. Chris mentioned that Parks proposed a connection from the lot to Bible Camp Road in the past and that was met with much resistance from local residents.

Joan presented a schematic diagram of a typical right-in/right-out/left-in intersection, the proposed turning movements for Bible Camp Road. Motorists from the north or south will be able to turn onto Bible Camp Road, but all traffic leaving will have to turn right (south) and continue south or use the Exchange Street RAB to turn around and then travel north on US 51. Joan explained that between the RAB and Bible Camp Road, this 14-foot-wide center lane could be used by northbound traffic waiting to turn onto Bible Camp Road.

The right-in/right-out/left-in layout was also discussed as a possibility for the Babcock Park boat ramp access. Boats and trailers leaving the parking lot and wanting to travel north could more easily turn right and use the RAB at Exchange Street to reverse direction. Another possibility would be accessing the boat ramp lot from Bible Camp Road. Darren indicated this was explored previously without success. The right-in/right-out/left-in intersection layout at Bible Camp Road would improve flow onto US 51 and area residents may be more willing to accept the parking lot traffic onto Bible Camp Road if the right-out-only turning movement is implemented. Darren and Chris feel the proposed turning movements and TWLTL for storage of northbound traffic trying to enter the boat ramp access are acceptable. However, Darren expressed concern for the south-bound vehicles and the lack of a turn lane/space for queuing for vehicles entering the boat ramp access.

There is limited space between the boat ramp access and the bridge over the Yahara River. Joan indicated this could be looked at closer, but this would likely require additional R/W from the park along the west of US 51. Darren indicated that Parks would not be opposed to additional R/W acquisition along the park (west side of US 51) if southbound vehicles could have a right-turn lane into the parking lot. Chris mentioned that reconfiguration of the parking lot and access to it would be welcomed if traffic flow in and out of the lot can be improved.

- **Strand will review and look at options for providing turning lane/queuing space on US 51 for southbound vehicles.**
- c. Chris asked about the bridge and if there are any plans for replacement. Joan indicated there are no plans to replace the bridge in the near future and the EIS design profile matches existing conditions. Chris mentioned that a study of the Yahara River flow is underway by Dane County



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and it should be completed in December 2010. The preliminary indications are that the existing bridge is restricting river flow and the study will likely recommend increasing the existing 60-foot span to 100 feet. Joan confirmed that the Environmental Impact Statement (EIS) for US 51 will consider the Yahara River study and recommendations.

4. Joan initiated a discussion of the 4(f) status of Babcock Park and possible impacts to the park.

- a. Joan provided a summary of what a Section 4(f) property is, definitions of "use" of a 4(f) property, and what a de minimis impact is. Joan reviewed a June 1, 2010 Memorandum, prepared by Strand regarding the current design and impacts to Babcock Park camp sites. The sections through this area show about 8 feet of R/W acquisition and an additional 1 to 3 feet of TLE. An existing chain link fence would be removed and the existing row of trees between the road and the campsites is about 9 to 14 feet from the existing R/W. Grading for sidewalks and the back slope would get close to this tree line. Joan indicated that WisDOT would like Parks to review the plans and sections and determine if they would be in agreement with a de minimis impact finding.

Chris indicated that Parks would like to retain the trees because they provide some protection to campers from traffic. The group discussed the trees, many of which are ash trees and may be replaced. The group consensus was that the trees may be lost because of damage from fill and grading operations. Mike Hoelker indicated that WisDOT will make reasonable efforts to protect the trees during construction, but that there are no guarantees. If the trees die years after construction, it is very difficult for WisDOT to fund and execute any type of tree replacement that far after completion of the project. Darren and Chris indicated that Parks would be willing to do some underplanting and get replacement trees established in case the existing trees die off.

Darren asked if any additional drainage from the roadway would flow onto the camp sites. Joan confirmed that the sidewalk would be sloped toward the road and storm sewer would handle road drainage. No additional runoff should impact the campground.

Various options for a replacement fence or a barrier were discussed, from a new chain link fence to a concrete barrier wall or retaining wall. The group agreed that the best option may be a barrier/fence combination. The concrete barrier at the back of sidewalk would protect campers from errant vehicles that leave the road. A fence would keep children in the park and away from traffic and would also prevent pedestrians from walking into the campground. Darren expressed concern with the road getting closer to the campsites and indicated that on occasion campers do use the area between the row of trees and the chainlink fence. Mike Harried suggested a concrete barrier for safety with perhaps a retaining wall west of that to eliminate the 4:1 backslope. This would limit the TLE needed and perhaps not endanger the trees. Mike Hoelker indicated the barrier and wall can be made aesthetically pleasing/decorative and would also block the view of traffic from campsites. Darren indicated that he believes Parks would prefer the wall/fence combination.

Darren indicated that Parks follows state standards on campsite size and other campground features. Darren is unsure if the impacts to the campground will result in the state standards not being met in any way.

- **Strand will provide Parks with additional plan and profile maps and sections that detail proposed R/W and TLE requirements at Babcock Park.**
- **Parks will review the proposed design and determine if they will concur with a de minimis impact finding for Babcock Park and provide documentation of the concurrence (a de minimis letter).**
- **Parks will provide details on the barrier/fence that would be acceptable. Parks will determine if the campground will be able to remain in compliance with state standards given the proposed design.**
- ***Note added on September 21, 2010: meetings between WisDOT and FHWA that took place after the September 7, 2010 meeting with Parks and the Village of***



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McFarland have provided additional considerations on the typical section in the McFarland area. WisDOT will communicate with Parks and the Village regarding the outcome of these meetings.

5. Additional discussion of bike and pedestrian accommodations.

- a. Don Peterson asked about providing a pedestrian crossing at Burma Road and indicated that a significant number of pedestrians cross there now, including kids from the residential area to the west/southwest that are walking to school. Darren added that campground users also cross there to access bait and other items at the nearest convenience store. Joan indicated there is no crossing improvement planned for that intersection and no median to provide a refuge for crossers. The group agreed that with the current design and new sidewalk along the west side of US 51, pedestrians can walk north to Farwell Street and cross there where a crossing and median will be provided. Joan indicated that addition of a crossing at Burma can be evaluated but anticipates it would be difficult to provide because of the proximity to Farwell Street.
- b. Darren indicated that Parks currently maintains a trail through the park for public use and that the trail is used year-round. The public can access Burma Road from the trail (roughly 450 feet from US 51) and Darren suggested that perhaps the sidewalk bordering the park, along the west side of US 51, is not needed. Mike Hoelker indicated that providing bike and pedestrian facilities for urban sections is mandatory and very difficult to avoid. At a minimum, the footprint would need to be preserved for potential future sidewalk construction. Don thinks the sidewalk on the east side of US 51 north of Burma makes sense and also extending it south of Burma to the former Dairy Queen parcel. But extending it farther south, past the large retaining wall, to Exchange Street may not be useful. Don questions the need for it there.

Allan pointed out there are a couple paths along the east side of the Yahara River that many use to move from the Exchange Street residential area north to schools or northwest to US 51 and the park. Allan agrees that the sidewalk on the east side of US 51, south of the river, may not be needed. Joan pointed out that if we changed to a rural section south of the river, pedestrian requirements don't apply. Mike Hoelker indicated that he would like to discuss this issue internally with the WisDOT bike/pedestrian coordinator.

- **Mike Hoelker and Jeff will discuss the typical section issue internally and determine bike and pedestrian options in this area.**

Chris asked about the timing of the EIS and potential construction of US 51. Joan indicated the Draft EIS is 6 months to a year out. Mike Hoelker added that most likely the construction would be 10 to 20 years out, but that smaller, safety improvements and intersection improvements could be made in the interim, such as the Exchange Street RAB and other upgrades through McFarland.

Chris asked about cost sharing for the improvements. Joan indicated that the improvements proposed would be fully funded by WisDOT. Improvements discussed that are outside the roadway area and that benefit the park could be mitigation measures. If there are some "above and beyond" improvements that the Parks would like constructed with the US 51 project, those could be negotiated and funded by Dane County.

6. Next steps.

Joan indicated that archaeological investigations are ongoing and should be finished this fall. Preliminary results are expected by November and then meetings with the PAC and TAC will be held. A public information meeting (PIM) is planned for January. The Draft EIS should be completed later in the year.

7. "Stoughton Bypass" Alternatives and Viking Park.

- a. Joan described the background of how the "Stoughton Bypass" alternatives were developed and the input from the public. The alternatives now include either County B or County B South combined with County N, Spring Road West, Spring Road East, County B to Pleasant Hill Off Alignment, and Skaalen to Pleasant Hill Road.



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Joan asked if Parks could reconfirm that they have no objections to the continued evaluation of the County B South alternative (which impacts the 4(f) Viking Park). Joan utilized the Plan & Profile of the design for this alignment (provided previously to Parks) for discussion and pointed out the bridge, some wetland impacts, a RAB at County N, pedestrian and bike accommodations, and extension of the Viking Park bike path under the proposed County B to existing County B. Joan asked Parks to discuss and identify what mitigation would be acceptable for park impacts. Jenny indicated a full 4(f) evaluation will be required for Viking Park.

Darren asked if Stoughton has had any input on potential connections of its bike path(s) a potential new path on existing County B. The group was not aware of any specific comments from Stoughton on this. Mike Hoelker indicated that Stoughton supports the County B South alignment because it fits well with planned future expansion of the industrial park in the area and because it impacts fewer residences. Mike pointed out that WisDOT can be flexible with a potential bike path extension from Viking Park to the rail corridor and would consider an off-road alignment. The alignment and connections can accommodate both Parks and the City of Stoughton. Possible connections might be to one or several city trails coming from the south. Joan noted that an off-road path would create additional right-of-way impacts.

Joan asked if Parks could make a decision on what alignment (County B or County B South) it prefers before December. This will allow presentation of the information to the public at the January PIMs. Mike Hoelker suggested that Parks provide a typical bike path section and identify a route on the plans. That can be added to exhibits for the PIMs and public comments can be obtained.

- **Strand will send additional plan sheets to Parks to show County B from County N west to the rail corridor.**
- **Parks will provide a preferred typical section for the trail extension from Viking Park and identify a route for the path on the plans.**
- **Parks will provide a recommendation on which County B alternative it prefers.**

The action items from the meeting are shown on the next page and organized by WisDOT, Parks or Strand responsibility.



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Action items from September 7, 2010, Project Meeting with Parks and McFarland:

WisDOT Action Items:

1. Tom Kowproski will check on the Yahara River bridge "cat walk" funding status.
2. Mike Hoelker and Jeff Berens will discuss the typical section in McFarland and determine bike and pedestrian options in the area.

Dane County Parks Action Items:

1. Review the proposed US 51 design and impacts at Babcock Park and determine if concurrence with a de minimis impact finding can be provided. Documentation of the concurrence (a de minimis letter) would be required.
2. Provide details on the type of barrier/fence at Babcock Park that would be acceptable as a mitigation measure.
3. Given the current design and impacts to Babcock Park, determine if the campground will remain in compliance with state standards.
4. Provide a typical bike path section for a potential trail extension from Viking Park.
5. Identify on design drawings a preferred trail route.
6. Indicate which County B alternative is preferred (County B or County B South).

Strand Action Items:

1. If authorized, look at options for providing a turning lane/queuing space on US 51 for southbound vehicles entering the boat ramp access.
2. Provide Dane County Parks with additional plan and profile maps and sections that detail proposed RW and TLE requirements at Babcock Park.
3. Provide Dane County Parks with additional drawing sheets to show County B from County N west to the rail corridor.

Note added on September 21, 2010: meetings between WisDOT and FHWA that took place after the September 7, 2010 meeting with Parks and the Village of McFarland have provided additional considerations on the typical section in the McFarland area. WisDOT will communicate with Parks and the Village regarding the outcome of these meetings.



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Minutes
Project Meeting with
Dane County Parks and
Land and Water Resources Department (LWRD)

May 13, 2011, at 10:00 A.M. to 12:30 P.M.
LWRD Office

Participants:

Craig Pringle, WisDOT Project Manager
Kevin Connors, LWRD Director
Luke Hellermann, Strand Associates
Chris James, LWRD, Dane County Parks Planner
Joan Petersen, Strand Associates

Exhibits:

Typical sections and cross sections (at Babcock Park)
Roll plots of design with aerial background - US 51 at Babcock Park; County B alignments at Viking Park

Meeting Goals:

1. Discuss the current design of Alternative B through McFarland in relation to impacts at Babcock Park, and the alternatives along County B near Viking Park.
2. Understand Dane County Parks and LWRD concerns about the design and strategize on possible design refinements to reduce park impacts.
3. Discuss next steps that would allow DCP and LWRD to present the latest information to the Dane County Parks Commission.

Action items: Noted in **bold** throughout. Summary of all action items provided on last page.

1. Babcock Park.

Joan presented an overview of the design in the McFarland area and indicated that the approximate right of way (R/W) acquisition from Babcock Park is 0.9 acres on the west side of US 51 and 0.8 acres on the east side of US 51. This is based on the typical sections and profile as presented at the April 2011 public information meeting.

Park Entrance and Lock & Dam Area (between Station 485 and 495 LT)

This area has the most significant R/W taking due to the widened typical section for US 51. Plans show slopes at 4:1. A 10-foot wide path is used between Station 485 and 492 LT, at which point it tapers to a 7-foot wide sidewalk. Items to consider in this area to reduce the R/W impact include: add retaining wall, reduce width of path (if possible, need to meet standards).

A connection from the path to the parking lot north of the Yahara River on the west side is needed.

Kevin and Chris explained reasons why the parking lot north of the river is important to the park: the shore area and parking lot are used by the public for shore fishing; the sheriff's boat is kept nearby; lock & dam improvements are planned and will include a new building/shed near the northwest corner of the bridge (to house an electrical control box for the lock & dam control structure)

They voiced concerns about the location of the path adjacent to the roadway. The County keeps the current path south of the river plowed and open during the winter. Snow removal from the extensive length of path along the entire park frontage to US 51 is a concern because it is an additional maintenance cost. Craig confirmed that WisDOT will not remove snow from sidewalks or paths. Kevin and Chris were concerned about the slope of the driveway entrance to the parking lot at Station 495 LT. It is a 10 percent slope which meets WisDOT criteria, but they felt it was too steep. Chris asked if the path



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needs to be within WisDOT R/W. Craig indicated it does because it is FHWA's position that the path serve US 51 pedestrians and be within the highway R/W for access and maintenance.

- **Strand will look at ways to flatten the slope of the driveway but this may require raising the grade of the north portion of the parking lot.**

Shore Fishing and Bridge Improvements

Kevin indicated that a LWRD goal is to improve shore fishing access near the dam. LWRD may build a new handicap fishing platform and the cat walk is an important item so that pedestrians and canoeists can pass under US 51.

- **At the south side of the river on the west side of US 51, LWRD would like the path to branch off to the west and run along the shore at the existing handicap fishing platform and then connect to a proposed catwalk structure under the bridge.**

The results of the Yahara River flow/water level study are not yet available. Joan noted that if the profile of the new bridge needs to be raised based on study results, the roadway profile would need to be raised and this would increase impacts to the Park. Kevin does not anticipate the study will recommend raising the bridge. He anticipates a recommendation to increase the bridge span on the south side, which is a choke point on the river. Kevin indicated the dam won't be widened and hopes that the new bridge span can match the dam width. Graef is designing lock & dam improvements.

Exchange Street Roundabout and Overflow Parking Lot (between Station 462 and 495)

The current posted speed on US 51 is 40 mph through McFarland with the speed limit changing to 55 mph for southbound traffic near Bible Camp Road. The study team anticipates posted speed will remain at 40 mph, and 40 mph will likely extend another 1000 feet south to the Exchange Street roundabout (RAB). The RAB was discussed and Kevin pointed out that there will be a high volume of vehicles with trailers. Also, there is a wide range in the level of driver skill for both the recreational boaters and the truck drivers using US 51. Joan and Craig confirmed that the RAB will be designed with this in mind, will be properly sized and marked and the 2-lane RAB and will utilize mountable curb and truck apron as needed.

Chris asked about the overflow parking lot on the east side of US 51. The access point is at Station 481+50 RT, across from the entrance to the main boat launch parking lot. Chris and Kevin noted there are a lot of pedestrians crossing US 51 at this location and there needs to be a cross walk. Craig indicated that WisDOT wants to minimize mid-block pedestrian crossings that are not signalized, but that if people will be and are crossing there, the markings and refuge need to be provided. The wider median separating US 51 southbound/northbound traffic is north of the driveways, so a marked crossing might be better located there. However, she noted that north of the driveways, with the addition of the southbound right turn lane, a slotted right turn median may be needed to provide refuge between the south-bound lanes and the right turn lane. This would extend the slope intercepts further to the west in this area. Kevin indicated this would not be a problem.

- **Strand will investigate providing a crossing at the over-flow lot near STA 482+00. A slotted right turn median would be needed in combination with the crosswalk.**

Kevin asked about WisDOT providing sidewalk along the east side of US 51 from the overflow lot to a proposed catwalk under the bridge. The typical section already includes provision for a 5-foot terrace and 5-foot future sidewalk, so the slope intercepts shown on the plan are correct. It was agreed that sidewalk can be included on the east side between Station 481+50 to 488+50 RT. Kevin and Chris noted that the cross walk, the sidewalk from the overflow lot to the catwalk, and a new catwalk under the bridge are very important items and would be significant mitigation measures that WisDOT could provide.

- **Strand to include sidewalk on the east side of US 51 between Station 481+50 to 488+50 RT.**



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Campsite Area.

The impacts to trees and WisDOT commitment to tree replacement were discussed. Craig confirmed that tree replacement can be a 4(f) commitment/mitigation measure. Kevin and Chris both feel tree wells should not be used to try and save existing trees near the slope intercepts. Craig indicated new trees can be planted anywhere in the park.

- **Craig will investigate funding mechanisms for tree replacement and the funding timing. Can early planting be funded ahead of construction to get replacement trees established?**

Use of a retaining wall adjacent to the campsites would reduce R/W acquisition by about 4 to 6 feet and some trees might be saved. Chris will review the cross sections with this in mind and recommend whether a retaining wall should be used and if so, the extent. DCP will also review which trees would need to be replaced. No retaining wall is also an option.

The location of the sidewalk along the west side in relation to a potential retaining wall was discussed. The sidewalk could be at the top of the wall or at the bottom of the wall. It is assumed that wherever there is retaining wall a concrete barrier would be used to protect traffic from the elevation change. The path/sidewalk widens to 10 feet width near the office and would need to come down to the office if the path is on top of the wall.

Is a traffic barrier wanted for protection of the campsites? A concrete barrier adds about 2 feet width to the typical section. Beam guard or bollards that provide more of visual block like screening with plants rather than a barrier could also be considered.

- **LWRD action items include the following:**
 - **Recommend the extent of retaining wall desired based on the cross sections.**
 - **Recommend a preferred facing type for the wall.**
 - **Determine if a concrete barrier, other barrier, fence, or vegetative screening is wanted on top of the retaining wall.**
 - **Decide where the sidewalk will be located and where it should connect to the park office.**
 - **Determine which trees need to be replaced, given the decisions on the retaining wall and path.**
- **Strand will send Chris a specification on retaining wall anti-graffiti coating.**

Craig suggested that LWRD identify the mitigation measures desired and WisDOT will review and respond.

2. Viking Park

Joan noted the County B on-alignment option (between Williams Drive and Lunde Circle) results in 6 residential relocations and relocation of the Dane County Highway shop. There would be substantial impacts to the remaining homes on the south side of County B because of the proximity of slope intercepts and steep slopes to account for the topography of the area. With the County B on-alignment option, Viking Park would be avoided completely. If the County B South option is chosen, R/W acquisition is needed from Viking Park, a 4(f)/6(f) site, and there are no residential relocations. Joan indicated that comments from the April 2011 PIM showed public support of the County B South alignment (9 comments supported the County B South option, 1 comment supported the on-alignment, 2 comments were opposed to the on-alignment). Chris indicated that with these comments from the public, he feels the Commission will be able to take a position on which alignment they endorse. Chris thinks the Parks Commission will want to know where the commenters reside.

- **Strand will review comment sheets for addresses of commenters on the County B options.**



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Replacement Lands and Mitigation.

Joan indicated that she heard the County Highway Garage site will be sold. Kevin indicated he heard the structure will be re-roofed and the County plans to keep the site for now. In general, Chris and Kevin identified replacement land and mitigation items that LWRD is interested in discussing with WisDOT. These include: acquiring the garage site for a future boat launch site; acquiring the parcel to the north of the garage for park expansion (owned by Arnett); extension of the trail on both the east and west sides of the river at Viking Park to the north under the proposed bridge; and WisDOT maintaining the existing County B bridge for use by pedestrians/bicycles. Craig suggested that LWRD identify the replacement land and/or other mitigation measures are desired and WisDOT will review and respond.

Chris and Craig have both been told by Mr. Arnett that he plans to move ahead quickly with his development plan for the land north of the highway garage site. It is unclear how quickly this development might materialize. Chris indicated that Mr. Arnett is interested in selling part or all of the parcel to WisDOT, or other options such as selling the shoreline to accommodate a trail extension from the park to his parcel.

- **Craig will investigate the highway garage site status; he will discuss potential early acquisition with WisDOT Real Estate and what schedule can be accommodated for Viking Park replacement lands and potential mitigation.**

Chris indicated that he has talked to the City of Stoughton and the City concurs with the County's plans for expansion of the park. Chris also has the impression that the City is not that excited about the planned senior housing development on the Arnett property. Chris recommends checking with the City on their plans for path extensions to the north. Can an extended City trail also go under proposed County B at the new bridge and connect to existing County B and the County trail?

- **Strand will check the City of Stoughton's trail extension plans.**
- **Strand will check bridge span needed to accommodate a trail underpass on the west side of the river. Discussion with DNR should be held regarding whether wetlands need to be spanned as well. What is the added cost?**

3. Next Steps: Design Revisions for Review Preparatory to Park Commission Meeting

Kevin indicated safety is a major concern for the Park Commission. Chris indicated the next Parks Commission meeting where these issues can be discussed is the 2nd Wednesday in July (July 13, 2011). Chris and Kevin indicated they anticipate the Commission will follow their recommendations with regard to Babcock and Viking Parks. Revising the design to address the topics discussed and adding additional information to the roll plots will be helpful for the Commission. Revisions to the design will be made after decisions by WisDOT and LWRD on the outstanding items.

See the next page for a summary of all action items.



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SUMMARY OF ACTION ITEMS

LWRD action items include the following:

1. Recommend the extent of retaining wall desired based on the cross sections.
2. Recommend a preferred facing type for the wall.
3. Determine if a concrete barrier, other barrier, fence, or vegetative screening is wanted on top of the retaining wall.
4. Decide where the sidewalk will be located and where it should connect to the park office.
5. Determine which trees need to be replaced, given the decisions on the retaining wall and path.
6. Identify the mitigation measures desired and provide to WisDOT for review.

WisDOT will:

1. Craig will investigate the highway garage site status; he will discuss potential early acquisition with WisDOT Real Estate and what schedule can be accommodated for Viking Park replacement lands and potential mitigation.
2. Investigate funding mechanisms for tree replacement and the timing of funding. Can early planting be funded ahead of construction to get replacement trees established?

Strand Associates will:

1. Review placing a connection from the path to the parking lot north of the Yahara River on the west side.
2. Look at ways to flatten the slope of the driveway - this may require raising the grade of the north portion of the parking lot.
3. Investigate connecting the path at the south side of the river on the west side of US 51 to branch off to the west and run along the shore at the existing handicap fishing platform and then connect to a proposed catwalk structure under the bridge.
4. Investigate providing a crossing at the over-flow lot near STA 482+00. A slotted right turn median would be needed in combination with the crosswalk.
5. Include sidewalk on the east side of US 51 between Station 481+50 to 488+50 RT.
6. Send Chris James a specification on retaining wall anti-graffiti coating.
7. Review comment sheets for addresses of commenters on the County B options.
8. Check the City of Stoughton's trail extension plans.
9. Check bridge span needed to accommodate a trail underpass on the west side of the river, estimate the additional cost, and discuss wetlands at crossing with DNR (span wetlands required?)
10. Revise the aerial plot following redesign efforts to show the following:
 - a. Existing and proposed R/W.
 - b. Sanitary force main east of US 51 and river crossing.
 - c. Do not show boat launch ramp connection to Bible Camp Road.
 - d. Retaining walls.
 - e. Confirm sidewalk width standards and minimize.
 - f. Use color to highlight sidewalk, terrace and median areas.
 - g. Crosswalk at approximate STA 482+00 and slotted right (if WisDOT agrees it is acceptable).
 - h. Utilize a longer bridge span to match the existing dam width.
 - i. Indicate future catwalk structure under the bridge, on south bank of the river.
 - j. Show sidewalk from overflow lot to the catwalk.
 - k. Identify trees that will be removed.
11. Send a pdf of the roll plots to LWRD for review.
12. After comment from WisDOT and LWRD, Strand will provide a large scale aerial plot for use by the LWRD in the July 13, 2011 meeting with the Parks Commission.



Dane County Land & Water Resources Department

Lakes & Watersheds • Land Acquisition • Land Conservation • Parks

Kevin F. Connors, Director

August 24th, 2011

Wisconsin Department of Transportation
Attn: Craig Pringle
2101 Wright Street 55L
Madison, WI 53704

Re: US 51 Corridor Study
Proposed CTH B Alignment

Dear Mr. Pringle;

On July 27th, the Dane County Park Commission met to review improvements being proposed on US 51 and CTH B that potentially will impact Babcock and Viking County Parks. Due to the scale and linear nature of both of these parks, we anticipate the effects of the proposed road expansion projects and increased traffic volumes to be considerable and detrimental.

Patrons who visit these parks are seeking respite from the urban environment and any loss of green space and additional non-park development will detract from a park user's experience. In addition, Dane County Parks relies on user fees generated from activities such as camping and dog parks to help offset operation and maintenance costs. Any changes to the surrounding landscape that negatively impact these uses have a very real chance of decreasing our annual revenues. Lastly, and perhaps most importantly, these park lands have been purchased using public dollars with the intent of preserving our natural resources and fragile wetlands in perpetuity. The Park Commission oversees stewardship of all Dane County park lands and must take extreme care when considering any proposals that would reduce or negatively impact existing acreage in our system.

Due to the significant deterioration that both Parks will experience as a result of the US 51 project, the Park Commission must receive assurance from WI DOT that the following list of mitigating activities will be honored. These activities are absolutely necessary for the Park Commissions' consent to any loss of parkland at Babcock and Viking County Parks. The Park Commission has developed this list as a way to preserve their significant natural resources and maintain use of both Parks as major recreation destinations that attract visitors from around the region and support the local economy.

BABCOCK COUNTY PARK

General Comments Babcock Park:

- Phase project to begin proposed improvements between Burma Road and Exchange Street within a 5-year time frame.
- Include provisions for wayfinding signage to park, campground and boat launch for north and south bound traffic.
- Replace trees lost from construction; location, size, type to be determined.

Lock/Dam and Shower Building Parking Areas

- Include relocation/recalibration of USGS station
- Provide connector path from proposed 51 path to parking lot/existing park path.

Lyman F. Anderson Agriculture & Conservation Center
1 Fen Oak Court, Room 208, Madison, Wisconsin 53718-8812 - PH: 608/224-3730, FAX: 608/224-3745
www.countyofdane.com/lwrd

- Reconstruct lock parking lot as single loaded on west side and expand north to storage sheds.
- Need retaining wall from station 489-494 that includes transition ramp/trail to parking lot grade.
- Raise and reconstruct parking lot at shower building to decrease entrance drive slope, include required storm water facilities.
- Provide pedestrian underpass from parking lot to east side of road for fishing/pedestrian shore access.

Bridge and Overflow Parking Lot Area

- Expand bridge cross section to the south at minimum to be same width as dam structure.
- Provide accessible access path from proposed 51 path south of bridge to existing fishing pier, dam.
- Provide cross walk to overflow lot on east side of 51 to boat launch with pedestrian refuge islands.
- Provide pathway from overflow parking area on east side of 51 to river for shore fishing and canoe/kayak access.
- Provide pedestrian walkway on east side of bridge deck, bicycle/pedestrian path on west side of bridge deck.
- Provide retaining wall from Station 478 + 50 to 481 to maintain existing boat landing parking lot roadway geometry.

Campsite Area

- Provide barrier wall minimum 8' height from Burma Road to park shower building. Angle north and south end sections to accommodate park signage. Offset barrier wall from proposed 51 path (distance to be determined) to allow green space for snow storage.
- Include aesthetic and informational provisions to be coordinated/approved by Dane County on barrier wall; ex: decorative concrete textures, artwork, park signage, etc.

It is anticipated that even with the above campsite area mitigation measures in place, future expansion of USH 51 will significantly diminish the quality of camping at Babcock County Park. In response, the Dane County Park Commission requests that the WDOT fund development of a campground facility on Lake Waubesa at Capital Springs Centennial State Park.

VIKING COUNTY PARK

Page 4-Viking Park

- Acquire Dane County Highway garage site for use by Dane County Parks.
- Establish an acquisition fund for use by Dane County to acquire lands for natural resource protection and recreation in the area near Viking County Park.
- Investigate and propose sound mitigation measures for the proposed bridge.
- Accommodate City of Stoughton and Dane County proposed bicycle/pedestrian and snowmobile trails.

As WDOT continues to refine preferred alternative scenarios and park mitigation discussions with Dane County, the above requests and recommendations should be codified within a legally binding Agreement. This Agreement would need to be executed by both parties before Dane County would approve any use of park lands for the proposed roadway expansion projects. We look forward to continuing discussions with the Department and wish you the best of luck on your project.

Sincerely,

William Lunney; Dane County Park Commission, Chair



Division of Transportation
System Development
Southwest Region
2101 Wright Street
Madison, WI 53704-2583

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Mark Gottlieb, P.E., Secretary
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October 14, 2011

Mr. William Lunney, Chair
Dane County Parks Commission
Lyman F. Anderson Agriculture & Conservation Center
1 Fen Oak Court, Room 208
Madison, WI 53718-8812

Re: Potential Babcock Park Impacts
US 51 EIS, Stoughton–McFarland
I-39/90 to US 12 (Madison South Beltline)
WisDOT Project ID 5845-06-02

Dear Mr. Lunney:

On August 31 and September 13, 2011, the Wisconsin Department of Transportation (WisDOT) and the Federal Highway Administration (FHWA) discussed the mitigation measures proposed by the Dane County Parks Commission (Commission) in your letter dated August 24, 2011. This letter is in response to the proposed Babcock Park mitigation measures.

The proposed mitigation measures in your letter included the items shown in italics. WisDOT's response follows.

1. *Phase project to begin proposed improvements between Burma Road and Exchange Street within a 5-year time frame.*

Funding has not been allocated for any US 51 project improvements and it is uncertain when funds might be available. Therefore, WisDOT cannot commit to beginning any improvements within a 5-year time frame.

2. *Include provisions for wayfinding signage to park, campground, and boat launch for north- and southbound traffic.*

WisDOT is willing to provide the signage requested.

3. *Replace trees lost from construction; location, size, type to be determined.*

WisDOT is willing to replace trees lost because of construction with the location (within Babcock Park), size, and type to be determined.

4. *Include relocation/recalibration of USGS station.*

WisDOT is willing to relocate/recalibrate the USGS station.

5. *Provide connector path from proposed 51 path to parking lot/existing park path.*

WisDOT is willing to provide this path connection.

6. *Reconstruct lock parking lot as single loaded on west side and expand north to storage sheds.*

WisDOT is willing to modify the lock parking lot as requested.



7. *Need retaining wall from station 489-494 that includes transition ramp/trail to parking lot grade.*
WisDOT is willing to provide this retaining wall and transition ramp/trail.
8. *Raise and reconstruct parking lot at shower building to decrease entrance drive slope, include required stormwater facilities.*
WisDOT is willing to discuss options for the entrance drive slope to the shower building parking lot. If needed, the parking lot will be raised and reconstructed with required stormwater facilities.
9. *Provide pedestrian underpass from parking lot to east side of road for fishing/pedestrian shore access.*
WisDOT has determined that it is not feasible to construct an underpass without substantially raising the grade of the bridge and US 51 north and south of the bridge. Raising the grade would result in greater impacts to Babcock Park on both the east and west sides of US 51. Additional discussion of this proposed mitigation measure is needed.
10. *Expand bridge cross section to the south at minimum to be same width as dam structure.*
WisDOT is willing to expand the bridge span as requested.
11. *Provide accessible access path from proposed US 51 path south of bridge to existing fishing pier, dam.*
WisDOT is willing to provide this path.
12. *Provide crosswalk to overflow lot on east side of 51 to boat launch with pedestrian refuge islands.*
WisDOT is willing to provide this crosswalk.
13. *Provide pathway from overflow parking area on east side of 51 to river for shore fishing and canoe/kayak access.*
WisDOT is willing to provide this path.
14. *Provide pedestrian walkway on east side of bridge deck, bicycle/pedestrian path on west side of bridge deck.*
WisDOT is willing to provide this walkway.
15. *Provide retaining wall from Station 478+50 to 481 to maintain existing boat landing parking lot roadway geometry.*
WisDOT will provide this retaining wall.
16. *Provide barrier wall minimum 8-foot height from Burma Road to park shower building. Angle north and south end sections to accommodate park signage. Offset barrier wall from proposed 51 path (distance to be determined) to allow green space for snow storage.*
WisDOT will provide this wall.



-
17. *Include aesthetic and informational provisions to be coordinated/approved by Dane County on barrier wall; ex: decorative concrete textures, artwork, park signage, etc.*

WisDOT is willing to provide some aesthetic and informational provisions on the wall and will coordinate these with Dane County Parks.

18. *Dane County Parks Commission requests that WisDOT fund development of a campground facility on Lake Waubesa at Capital Springs Centennial State Park.*

Satisfying this proposed mitigation measure is not possible because WisDOT is not able to provide money as a mitigation measure.

We understand your concerns with the US 51 project's potential impacts to Babcock Park and we appreciate your consideration and identification of reasonable mitigation measures. We look forward to discussing these ideas with you further and identifying mutually agreeable mitigation measures that will allow the US 51 project to move forward.

Sincerely,

Craig Pringle

Craig Pringle, P.E.
Project Manager

- c: Johnny Gerbitz, FHWA
Mike Hoelker, WisDOT SW Region
Jennifer Fredrickson, WisDOT SW Region
Brian Taylor, WisDOT SW Region
Sharlene TeBeest, WisDOT Central Office
Lavane Hessler, Financial Assistance Specialist, DNR
Joan Petersen, Strand Associates, Inc.®



WisDOT ID 5845-06-02
US 51 Environmental Impact Statement
I-39/90 to Madison South Beltline
Dane County

Project Meeting with
Dane County Parks (DCP) and
Land and Water Resources Department (LWRD)

October 31, 2011, at 8:30 A.M. to 10:00 A.M.
LWRD Office

Participants:

Craig Pringle, WisDOT Project Manager
Kevin Connors, LWRD Director
Luke Hellermann, Strand Associates
Shar TeBeest, WisDOT
Jennifer Fredrickson, WisDOT
Brian Taylor, WisDOT

Chris James, DCP Parks Planner
Joan Petersen, Strand Associates
Michael Hoelker, WisDOT
Darren Marsh, DCP Director
Tom Koprowski, WisDOT
Laura Guyer, DCP Conservation Fund Manager

Exhibits:

Roll plot of current design of US 51 at Babcock Park
Roll plots showing alignments of County B under consideration at Viking Park
August 24, 2011 letter from DCP requesting mitigation measures at Babcock and Viking Parks
October 14, 2011 response letter from WisDOT to DCP regarding Babcock Park mitigation
October 14, 2011 response letter from WisDOT to DCP regarding Viking Park mitigation

Meeting Goals:

1. Discuss the current design of Alternative B through McFarland in relation to impacts at Babcock Park and mitigation measures.
2. Discuss the alignments of County B under consideration at Viking Park, Park impacts, and mitigation measures.
3. Next steps to move forward with completion of Draft EIS.

Action items: Noted in **bold.**

1. Viking Park

Joan presented a new alignment of County B that would avoid impacts to Viking Park. The alignment is the same as the previously presented County B South alignment in that it goes off alignment south of existing County B near Williams Drive. As the new alignment approaches Viking Park it curves north, returning to existing County B and avoiding the park. The October 14, 2011 response letter from WisDOT to DCP with the requested Viking Park mitigation measures was discussed, item by item:

Item 1 - Acquire Dane County Highway Garage.

Craig and Michael explained that timing/acquisition of the requested 6(f) replacement land (Highway Garage Site) is a problem. WisDOT can't commit to the acquisition without funding of a construction project. This is a planning study and it is unknown when funding might be allocated for construction. The property can't be acquired now and the future owner may not be a willing seller. WisDOT can't condemn for 6(f) replacement land.

Craig explained that the County B South alignment was originally developed based on public comments and the public's desire to avoid impacts to residential properties along County B and the residential relocations at the County B/County N intersection. This revised alignment will still have some relocations at the County B/County N intersection, but if the park can be avoided with an alignment that is feasible and prudent, that must be evaluated.



WisDOT ID 5845-06-02
US 51 Environmental Impact Statement
I-39/90 to Madison South Beltline
Dane County
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Laura asked if the planned development north of the Garage site by the City of Stoughton, had anything to do with the development of this new alignment that avoids the Park. Craig indicated it did not. Laura indicated that the Commission also wanted additional lands north of the Garage site as part of the mitigation and that this was requested as Item 2 in their letter (Establish and acquisition fund to acquire lands...). Michael noted the DNR's 6(f) expert indicated 6(f) replacement land could also be provided west of the river or at the south end of the Park. Shar pointed out that any 6(f) replacement land would be of equivalent use to the land taken and typically this is at or close to a 1:1 ratio.

Darren indicated DCP would like connectivity of trails. Michael noted that the new roadway would have on-road bike/pedestrian facilities and DCP could tie into those. Darren pointed out that a snowmobile trail uses the existing bridge to cross the river. Michael indicated the new roadway and bridge can be constructed to accommodate bike, pedestrian, and snowmobiles. WisDOT will work with DCP and the city of Stoughton to determine the priorities and how to accommodate each. Darren indicated the existing snowmobile trail is a main corridor, state-funded trail and that snowmobiles are to be 40 feet off the road way for night travel.

- **The new roadway and bridge, regardless of the alignment selected, will accommodate bikes and pedestrians. The new bridge will also accommodate a snowmobile crossing of the river.**

Item 2 - Establish an acquisition fund for use by Dane County to acquire lands...

Joan indicated that this requested mitigation is not possible because WisDOT can't provide financial mitigation.

Item 3 - Investigate and propose sound mitigation...

Joan and Michael indicated that noise analysis would be completed now for the EIS and then would need to be updated prior to construction because there would be more than a 3 year delay. It is agreed that noise mitigation may be needed. Shar indicated that FHWA determines whether or not noise levels result in an impact to a 4(f) resource.

Item 4 - Accommodate Stoughton and Dane County bicycle/pedestrian and snowmobile trails.

This was discussed previously and accommodations will be provided.

- **Darren or Chris will provide a markup of the plans to show their preferred route for the snowmobile trail and river crossing.**
- **Dane County Parks will provide a response to these alignments and the mitigation measures WisDOT can provide.**

2. Babcock Park.

The October 14, 2011 response letter from WisDOT to DCP with the requested Babcock Park mitigation measures was discussed, item by item:

Item 1 - Phase project to begin proposed improvements between Burma Road and Exchange Street within a 5-year time frame.

Michael indicated that WisDOT can't commit to this because construction of the preferred alternative will not take place within 5 years. Some spot improvements/safety improvements like reconstruction the Exchange Street intersection may happen sooner, but there is no money currently programmed. Michael estimated roughly that corridor construction would not occur until after 2020 unless priorities change and money is made available.

- **WisDOT will e-mail the currently anticipated, approximate timeline for the EIS and construction of improvements to Darren and Chris.**



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Items 2 through 8.

WisDOT is willing to provide these requested mitigation measures.

Item 9 - Provide pedestrian underpass from parking lot to east side of road for fishing/pedestrian shore access.

Joan indicated the underpass cannot be constructed without a sever grade change and major retaining wall system to provide sufficient clearance under the bridge. Craig added that the accommodation that will be provided is the cross walk from the overflow parking lot to the main lot, about 600 feet south of the bridge. People can portage at that location and signs can be added to designate the crossing as the portage location.

Items 10 and 11.

WisDOT is willing to provide these requested mitigation measures.

Item 12 - Provide crosswalk to overflow lot on east side of 51 to boat launch with pedestrian refuge islands.

WisDOT is willing to prove this crosswalk. The crosswalk will likely be pedestrian-activated. Sufficient pedestrian refuge will be provided in the median and at the right turn lane.

Items 13 through 17.

WisDOT is willing to provide these requested mitigation measures.

Item 18 - Dane County Parks Commission requests that WisDOT fund development of a campground facility on Lake Waubesa at Capital Springs Centennial State Park.

Joan indicated that this requested mitigation is not possible because WisDOT can't provide financial mitigation.

Darren and Kevin indicated they would like a sidewalk on the east side of US 51 from the bridge north to Yahara Drive. DCP is concerned about snow removal and snow storage. Michael indicated that sidewalks require snow removal, paths do not.

Because of the snow removal requirement, DCP would prefer construction of paths on the park property rather than sidewalks. On the west side of US 51, along Babcock Park, DCP agrees that the design should stay as is with a 5-foot sidewalk and a terrace for snow storage. DCP will negotiate with McFarland to determine snow removal requirements and who will be responsible for snow removal.

- **WisDOT will consider revising the design to include sidewalk on the east side of US 51 from the bridge to Yahara Drive. This is not DCP property and the Village of McFarland should be consulted.**

3. Next Steps:

- **DCP will coordinate with the Park Commission and request a response on the County B alignments and the mitigation measures WisDOT can provide at both Viking and Babcock Parks.**
- **WisDOT will attend Commission meeting on 11/16/11.**



Dane County Land & Water Resources Department
Lakes & Watersheds • Land Acquisition • Land Conservation • Parks

Kevin F. Connors, Director

November 28, 2011

Wisconsin Department of Transportation
Attn: Craig Pringle
2101 Wright Street 55L
Madison, WI 53704

Re: US 51 Corridor Study
Proposed CTH B Alignment

Dear Mr. Pringle;

On November 16, 2011, the Dane County Park Commission reviewed the mitigation responses that were sent from you to Park Commission Chair Bill Lunney on October 14, 2011 regarding possible impacts to Babcock and Viking County Parks from the expansion of USH 51 and realignment of CTH B.

Although disappointed that a pedestrian underpass and relocated campground facility were not able to be provided, the Commission was generally in agreement with additional mitigation measures being proposed at Babcock County Park.

Regarding Viking County Park, the Commission maintains that the following mitigation measures would need to be provided by the Wisconsin Department of Transportation for future consideration of an alignment that would travel through the park:

- Acquire Dane County Highway garage site for use by Dane County Parks.
- Establish an acquisition fund for use by Dane County to acquire lands for natural resource protection and recreation in the area near Viking County Park.
- Investigate and propose sound mitigation measures for the proposed bridge.
- Accommodate City of Stoughton and Dane County proposed bicycle/pedestrian and snowmobile trails.

Please continue to keep us updated on the status of your project.

Sincerely,

Darren Marsh
Director, Parks Division

Cc: Bill Lunney, Chair, Dane County Park Commission
Kevin Connors, Director, Dane County Land and Water Resources Department

Lyman F. Anderson Agriculture & Conservation Center
1 Fen Oak Court, Room 208, Madison, Wisconsin 53718-8812 - PH: 608/224-3730, FAX: 608/224-3745
www.countyofdane.com/lwrp

**DANE COUNTY BOARD OF SUPERVISORS
MINUTES OF THE MEETING OF THE
DANE COUNTY PARK COMMISSION**

Date and Time: Wednesday, February 27, 2013 – 5:30 p.m.

Location: 5201 Fen Oak Drive – Room 221
Madison, WI 53718

Members present: Christine Haag, Chair Bill Lunney, Supervisor Kyle Richmond, Supervisor Dave Ripp, Tom Thoresen, Dan Wisniewski (5:35 pm) Mary Schlaefer (5:35 pm)

Members excused:

Staff present: Chris James, Sara Kwitek, Darren Marsh

Others present: Brenda Konkol, Jeff Berens, Michael Hoelker, Jenny Grimes

1. Call Meeting to Order

Meeting called to order by Chair Lunney at 5:30 pm

2. Approval of Minutes

Motion by Ripp, second by Thoresen, to approve the December 12, 2012 minutes.

M.C. 4-0

3. Public Comment

Brenda Konkol – stated that she was representing the campers at Token Creek Park and wanted to know when the County would respond to several questions that were asked by Bruce Wallbaum in his reply to the County's offer to extend the Special Camping Permit for the park. The questions included: will the county provide the truck necessary to move the campers back to the City of Madison when the permit comes to an end, and will the county be flexible at the end of the permit if there is extreme weather conditions. Brenda also stated campers would volunteer to clean up the site if given direction on what to do.

4. Announcements and Correspondence

- Upper Mud Lake Proposed Land Purchase (Gramm's) - Press event was held on the opening of the Madison Fishing Expo at the Alliant Energy Center. County Executive Joe Parisi and Park Chair Bill Lunney spoke at the event. James stated that there is tremendous support for securing a safe public access to Upper Mud Lake referencing the acquisition priority in the County Parks and Open Space Plan and the State/County - Capital Springs Recreation Areas Master Plan.
- New County Parks Brochure – James handed copies out of the new park map that was recently completed.

5. Res. 256, 2012-2013. Authorizing the Acquisition of Land for Access to Upper Mud Lake

Motion by Thoresen, second by Wisniewski to recommend approval of the Resolution 256, 2012-2013 to the County Board for the purchase of land providing access to Upper Mud Lake.

M.C. 7-0

6. Highway 51 Planning Update – Babcock County Park

Jeff Berens updated the Commission on the status of the US 51 project in the Village of McFarland that is proposed to result in widening the roadway to be approximately 17' closer to the Babcock County Park campground. For the past two years the WDOT and Dane County Parks have been working on mitigation measures in exchange for the additional right of way needed for road expansion. Because of the presence of archaeological resources in the park, the Federal Highway Administration has indicated they would not be able to offer relocation of campsites adjacent to the highway as one of the mitigation measures. Jeff stated in order to keep the highway expansion planning project on schedule, the WDOT is going to move forward with the required Section 4F analysis that will include all of the previous agreed upon mitigation measures except for the campsite relocation. The campsite relocation issue would be addressed in the future during the right of way real estate appraisal process. The Commission asked that staff prepare a draft Memorandum of Understanding between Dane County and the WDOT that acknowledges the necessity to move the planning process forward and that the campground relocation mitigation measure is still unresolved, the anticipated project timeline, the Commission's desire to relocate campground sites within the park if possible, or if not, consider another mutually agreeable relocation site.

Motion by Schlaefer, second by Wisniewski reconfirming the Park Commission's position there is not a de minimis impact to the park unless all sixteen mitigation measures are provided including the campsite relocation.

M.C. 7-0

7. PARC Grant Program Application Materials and Timeline
Kwitek provided an overview of the 2013 PARC Grant Program stating that there was one million dollars allocated for 2013. The maximum grant amount is \$250,000. The grant deadline is tentatively set for May 3, 2013 providing two months for parties to submit an application. This is a little later than the deadline used last year. Last year the Commission awarded grant in June. This should still provide enough time for projects to get started by late summer or fall. Grant recipients are given two-years to complete their projects. Schlaefer stated that she was concerned that our grant evaluation is not weighted enough to justify its regional significance. Regional significance was one of our primary considerations for providing county money towards a project. Schlaefer offered two options for making sure that a project meets regional significance: one, have regional significance as the first criteria uses to determine the eligibility of a project before it moves on for further evaluation; or give more points (weight) within the current format under the regional evaluation. Wisniewski agreed, added that a simple fix that could be considered was to add more points to question #14. The Commission asked staff to consider adding more points to question #14 and possible modification of the question to allow for the greater number of points.
8. Park Endowment
Lunney provided an update on the work of the Fundraising and Endowment Development Subcommittee stating that the subcommittee has met a number of times and put in considerable time discussing and exploring the creation of a park endowment. He stated that the Park Commission has been invited by the Madison Community Foundation to submit a proposal for a matching campaign to establish an endowment to support the county parks. Lunney stated he considered this an opportunity to create a long-term alliance with the Madison Community Foundation (MCF) who has been a tremendous supporter of the county park system. The Friends of Dane County Parks has dissolved and wishes to contribute \$15,000 towards an endowment and non-designated Dane County Park donations can provide an additional \$15,000 or more towards the startup. There are opportunities to campaign for additional donations with upcoming events such as the dedication of Nine Springs E-Way for the Lewis's, planned for this spring. Supervisor Richmond agreed with Bill's assessment of the subcommittee's common goal towards establishing an endowment. Staff provided a list of other communities within Wisconsin that are currently using endowments.

Motion by Haag, second by Richmond requesting that staff pursue the development of a partnership with the Madison Community Foundation by submitting an application for matching campaign to develop a fund to support Dane County Parks.

M.C. 7-0

9. Brigham Bike Trail Project
James updated the Commission on a trail project that began this winter preparing a corridor for a new bike/pedestrian trail from the Military Ridge State Trail to Brigham County Park. Park staff are removing the trees within the corridor that will be utilized for a new shelter the county hopes to construct at Brigham County Park later this year. The shelter will be a timber frame structure similar to the one constructed at the Scheidegger Forest.
10. Any Other Business Allowed by Law
Next Meeting will be March 13, 2013.
11. Adjourn
Meeting adjourned at

Recorder: Darren Marsh

These minutes are the notes of the recorder and are subject to review, revision, and approval at a subsequent meeting of the Commission.

Hellermann, Luke

From: Berens, Jeff - DOT <Jeff.Berens@dot.wi.gov>
Sent: Wednesday, November 04, 2015 9:57 AM
To: Hellermann, Luke
Subject: FW: 5845-06-03 US 51 Stoughton to McFarland: Babcock Park Overflow Parking Lot

From: James, Christopher [mailto:James@countyofdane.com]
Sent: Wednesday, October 28, 2015 5:23 PM
To: Berens, Jeff - DOT <Jeff.Berens@dot.wi.gov>
Cc: Marsh, Darren <Marsh@countyofdane.com>; DOT DTSD US 51 Stoughton to McFarland EA <DOTDTSDUS51StoughtontoMcFarlandEA@dot.wi.gov>
Subject: RE: 5845-06-03 US 51 Stoughton to McFarland: Babcock Park Overflow Parking Lot

It all used to be gravel and has grown over in grass, so there is a stable base under the grass.

From: Berens, Jeff - DOT [mailto:Jeff.Berens@dot.wi.gov]
Sent: Wednesday, October 28, 2015 5:04 PM
To: James, Christopher
Cc: Marsh, Darren; DOT DTSD US 51 Stoughton to McFarland EA
Subject: RE: 5845-06-03 US 51 Stoughton to McFarland: Babcock Park Overflow Parking Lot

Chris – Is the overflow lot currently gravel? At this point I was thinking the new driveway would be gravel and we'd probably look to replace the rest in kind. If you'd like something different we could discuss. Thanks.

From: James, Christopher [mailto:James@countyofdane.com]
Sent: Wednesday, October 28, 2015 2:14 PM
To: Berens, Jeff - DOT <Jeff.Berens@dot.wi.gov>
Cc: Marsh, Darren <Marsh@countyofdane.com>; DOT DTSD US 51 Stoughton to McFarland EA <DOTDTSDUS51StoughtontoMcFarlandEA@dot.wi.gov>
Subject: RE: 5845-06-03 US 51 Stoughton to McFarland: Babcock Park Overflow Parking Lot

Jeff,
Would you be replacing the gravel base material over the parking area with the 20:1 alternative?
Thanks
chris

From: Berens, Jeff - DOT [mailto:Jeff.Berens@dot.wi.gov]
Sent: Wednesday, October 28, 2015 1:57 PM
To: James, Christopher
Cc: Marsh, Darren; DOT DTSD US 51 Stoughton to McFarland EA
Subject: RE: 5845-06-03 US 51 Stoughton to McFarland: Babcock Park Overflow Parking Lot

Chris – Just checking in to see if you have any questions on the different options for the overflow lot. Let me know if you have an option that Dane County Parks would prefer or if you'd like to meet to discuss things in more detail. Thanks.

Jeff Berens, P.E.
Planning Engineer
WisDOT SW Region - Madison Office

(608) 245-2656

From: Berens, Jeff - DOT
Sent: Wednesday, October 14, 2015 11:26 AM
To: 'James, Christopher' <James@countyofdane.com>
Cc: Marsh, Darren <Marsh@countyofdane.com>; DOT DTSD US 51 Stoughton to McFarland EA <DOTDTSDUS51StoughtontoMcFarlandEA@dot.wi.gov>
Subject: RE: 5845-06-03 US 51 Stoughton to McFarland: Babcock Park Overflow Parking Lot

Chris & Darren - Attached are the 20:1 option for Babcock Park and the 4:1 with no grading for Babcock Park. Sidewalk has also been put in to the new driveway. The new driveway has been widened to 30 feet (10 feet wider). Since it is a lot wider it has also been shifted slightly to the south to get a better entrance to the lot.

Notes:

- 1) The 20:1 option will allow cars to park about 8 feet closer to US 51 compared to the 4:1 slope.
- 2) R/W information for Babcock Park (all parcels)

Option	R/W Required	TLE Required	Total Affected Acres
20:1 Slopes	1.2	2.0	3.2
4:1 Slopes	1.2	1.7	2.9

Let me know if you have any questions. Thanks.

Jeff Berens, P.E.
Planning Engineer
WisDOT SW Region - Madison Office
(608) 245-2656

From: James, Christopher [<mailto:James@countyofdane.com>]
Sent: Tuesday, October 13, 2015 2:12 PM
To: Berens, Jeff - DOT
Cc: Marsh, Darren
Subject: RE: 5845-06-03 US 51 Stoughton to McFarland: Babcock Park Overflow Parking Lot

Jeff,
That sounds like a good alternative, including eliminating the 20:1 slopes.
Thanks
chris

From: Berens, Jeff - DOT [<mailto:Jeff.Berens@dot.wi.gov>]
Sent: Tuesday, October 13, 2015 11:44 AM
To: James, Christopher
Cc: DOT DTSD US 51 Stoughton to McFarland EA
Subject: 5845-06-03 US 51 Stoughton to McFarland: Babcock Park Overflow Parking Lot

Hi Chris,

At our August 26, 2015 Public Involvement Meeting (PIM) for the US 51 Stoughton to McFarland meeting we received a comment about the overflow parking lot to Babcock Park. He indicated that the overflow lot gets used on busy weekends by vehicles with trailers. He pointed out that based on the design shown at the meeting it would not be possible to park in the overflow lot and then drive straight across US 51 to get to the boat launch area to get a boat. The design shown at the meeting would require a driver to make a right turn out of the overflow lot, travel north to find a place to turn around, and then head south to the main lot/boat launch.

In order to address this concern we have developed an option where the entrance to the overflow parking lot would be moved approximately 250 feet to the south. This would allow anyone exiting the lot the option of getting into the northbound left-turn lane that would take them into the main lot/boat launch.

I have attached a graphic showing the relocated driveway along with some cross sections showing the lot graded out at 20:1. We anticipate it would require approximately 0.8 acres of temporary easement to remove the old entrance, put in the new entrance, and provide minor grading in the lot. Please note that if the County would prefer we could eliminate the 20:1 slopes and match into existing ground at a 4:1 slope off the back of sidewalk, which would reduce the temporary easement required.

Let me know if you have any questions, comments, or if you would like to get together to discuss the proposed changes. Thank you.

Jeff Berens, P.E.
Planning Engineer
WisDOT SW Region - Madison Office
(608) 245-2656



WisDOT ID 5845-06-03
US 51 Environmental Assessment
Stoughton-McFarland
I-39/90 to US 12/18
Dane County

DANE COUNTY PARKS MEETING MINUTES

August 12, 2019
1 to 2:00 P.M.
Dane County Parks
5201 Fen Oak Drive, Madison

Attendees:

Jeff Berens, Wisconsin Department of Transportation (WisDOT)–Southwest (SW) Region
Brandon Lamers, WisDOT–SW Region
Joel Brown, WisDOT–Central Office
Chris James–Dane County Parks
John Reimer–Dane County Land and Water Resources
Darren Marsh–Dane County Parks (arrived at 1:45 p.m.)
Joan Petersen, Strand Associates, Inc.® (Strand)
Jenny Kobryn, Strand

Exhibits: US 51 Roll Plot for Babcock Park area
Preliminary plan sheets for US 51 and adjacent Babcock Park with both 2016 and 2019 proposed right of way

1. Introduction

Jeff Berens provided an introduction and background on the proposed project. Act 59 removed the ability for WisDOT to acquire right of way for bike lanes and multiuse paths unless specific mitigation items already agreed on included bike or multiuse path accommodations. Previously proposed on-street bike lanes were removed from US 51 in the urban section of McFarland. The previously proposed multiuse path from County B (east) to Skyline Drive was also removed from the updated preliminary plans.

Chris James asked if improvements would occur on County B (east). Jeff responded that Alternative H is the preferred alternative which has no improvements along County B (east) east of the intersection with US 51.

2. Design Changes

Jenny Kobryn explained the changes to the design through Babcock Park. Chris asked if the 12-foot path to the dam would be wide enough for maintenance. John Reimer confirmed that 12 feet should be fine.

Chris asked if the parking lot on the north side of the Yahara River would be improved soon. The parking lot is in bad condition and was on Dane County Parks list of proposed improvements. John asked if there would be any other improvements besides new pavement. Specifically, oil and grease control. Jeff stated that there are currently no requirements to provide specific improvements at locations as long as the project meets all WDNR requirements. Chris stated that this would be a Dane County requirement. Joel Brown mentioned that we could discuss adding mitigation measures. The goal would be to have all parties agree to a *de minimis* impact on Babcock Park. Chris stated that placing a trench filled with rock would probably meet the Dane County specifications. Dane County would like the mitigation item for this parking lot to state that it will follow Dane County Ordinance Chapter 14. Strand will review the ordinance and WisDOT will respond to Dane County as soon as possible about this request.

Chris asked about the park signs and locations. Jeff stated that mitigation item No. 1 was to work with Dane County on all sign placement.

3. Yahara River Bridge

John mentioned that Dane County planned to dredge the Yahara River because of the flooding from Monona to Stoughton. He wanted to confirm that the abutments would be deep enough to accommodate annual dredging. Also, John requested that the bridge be widened to include the full river width. Jeff mentioned that



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Dane County

DANE COUNTY PARKS MEETING MINUTES

August 12, 2019
1 to 2:00 P.M.
Dane County Parks
5201 Fen Oak Drive, Madison

there is a mitigation item already agreed to the bridge would be widened to the width of the dam or larger. Joan noted the proposed abutment locations on the graphic. The depth of the new abutments is not yet know as it would be a final design element.

4. Flooding

John mentioned that there was a lot of flooding and a possible solution would be to place a pipe line under US 51 from the Yahara River at Babcock Park to Lake Kegonsa near County B/AB. The pipe would be approximately 60 inches in diameter. Brandon Lamers said he would ask WisDOT administrators if this would be allowed. John stated that the pipe would probably be consultant-designed with WDNR, UWM, and the existing task force as reviewers. Brandon asked if there were any documents that could be provided to the administration office to assist with the explanation. John stated that there is a technical report that he can send over.

5. De minimis

Darren Marsh stated that he thought WisDOT and Dane County Parks might be able to work toward a *de minimis* finding. He requested a letter that outlines the mitigation measures to take to the Parks Commission. Some of the commission members are new so it will probably take two meetings with them before a decision can be made.

Prepared by the Strand team and respectfully submitted.

DRAFT

From: [Berens, Jeff - DOT](#)
To: [Joel Brown](#)
Subject: FW: 5845-06-03 US 51 Stoughton to McFarland: Lower Yahara River Trail
Date: Tuesday, November 12, 2019 7:16:53 AM

From: James, Christopher <James@countyofdane.com>
Sent: Monday, November 11, 2019 3:14 PM
To: Berens, Jeff - DOT <Jeff.Berens@dot.wi.gov>
Subject: RE: 5845-06-03 US 51 Stoughton to McFarland: Lower Yahara River Trail

Jeff, got your message, let me know if this works:

Dane County Parks confirms the need to temporarily detour the Lower Yahara River Trail while the US 51 bridges over Taylor Road are under construction. Public notification, suitable detour routes and project timing should be coordinated with Public Works staff from the Village of McFarland.

Hope this helps,
chris

From: Berens, Jeff - DOT [<mailto:Jeff.Berens@dot.wi.gov>]
Sent: Monday, November 11, 2019 12:42 PM
To: James, Christopher
Subject: RE: 5845-06-03 US 51 Stoughton to McFarland: Lower Yahara River Trail

Chris,

WisDOT understands the Lower Yahara River trail is a collaborative effort between Dane County Parks and other jurisdictions including the village of McFarland. WisDOT also understands all infrastructure related to the trail is not owned or maintained by Dane County Parks. Since Dane County is leading the trail development effort and promoting the existing trail and future phases of the trail, WisDOT would like confirmation that Dane County Parks believes temporarily detouring the trail is appropriate while the US 51 bridges over Taylor Road are under construction. The specific location of the detour would be discussed with Dane County Parks and the village of McFarland during final design. Please let me know if you would like to discuss this further. Thank you.

Jeff Berens, P.E.
Major Studies Project Manager
WisDOT SW Region - Madison Office
(608) 245-2656

From: James, Christopher <James@countyofdane.com>

Sent: Friday, November 08, 2019 1:40 PM

To: Berens, Jeff - DOT <Jeff.Berens@dot.wi.gov>

Cc: 'Jim Hessling' <Jim.Hessling@mcfarland.wi.us>

Subject: RE: 5845-06-03 US 51 Stoughton to McFarland: Lower Yahara River Trail

Thanks Jeff, you will need to coordinate with the Village of McFarland on this, we don't operate/maintain the trail through the Village. I've copied Jim Hessling who can let you know who would be most appropriate to coordinate with.

chris

From: Berens, Jeff - DOT [<mailto:Jeff.Berens@dot.wi.gov>]

Sent: Friday, November 08, 2019 7:42 AM

To: James, Christopher

Subject: RE: 5845-06-03 US 51 Stoughton to McFarland: Lower Yahara River Trail

Chris,

Thank you for including the information in the e-mail below related to the Lower Yahara River Trail (LYRT) and its crossing of US 51 parallel to Taylor Road in the village of McFarland. If enumerated for construction, the US 51 Stoughton – McFarland Study includes work on the US 51 bridges over Taylor Road. When these bridges are under construction, a detour of the LYRT onto local streets in the village of McFarland would be required to maintain the safety of the users of the LYRT. This detour would be signed. Specific detour routes would be determined with input from with Dane County Parks during final design. Preliminarily it appears like utilizing Siggelkow Road and other various local roads in McFarland with bicycle and pedestrian accommodations would be appropriate. This detour would be temporary, long enough for construction activities on the bridges over Taylor Road. WisDOT does not anticipate any permanent impacts to the LYRT would result from construction activities.

Please respond to this e-mail and acknowledge Dane County agrees with the contents of this e-mail.

Thank you,

Jeff Berens, P.E.

Major Studies Project Manager

WisDOT SW Region - Madison Office

(608) 245-2656

From: James, Christopher <James@countyofdane.com>

Sent: Monday, November 04, 2019 11:35 AM

To: Berens, Jeff - DOT <Jeff.Berens@dot.wi.gov>

Subject: RE: 5845-06-03 US 51 Stoughton to McFarland: Lower Yahara River Trail

Hey Jeff,

Here's some information, yes the LYRT extends from the Lussier Family Heritage Center at Lake Farm Park to the City of Stoughton, it goes under US 51 in McFarland. Obviously not all of the trail has been developed yet, just starting planning for the County segment of Phase 2 now.

Hope this helps, let me know if you need more.

thanks

Chris

<https://parks-lwrd.countyofdane.com/Trail/Lower-Yahara-River-Trail>

From: Berens, Jeff - DOT [<mailto:Jeff.Berens@dot.wi.gov>]

Sent: Thursday, October 31, 2019 2:21 PM

To: James, Christopher

Subject: 5845-06-03 US 51 Stoughton to McFarland: Lower Yahara River Trail

CAUTION: External Email - Beware of unknown links and attachments. Contact Helpdesk at 266-4440 if unsure

Hi Chris,

Do you have an official map of the Lower Yahara River Trail that you could provide? I've done some checking on the trail website and it looks like it ends at McDaniel Park in McFarland, but I saw something that said the trail continues on into McFarland. I'm wondering if the county actually officially designated a trail through the McFarland area or if there are just routes that extend from the trail into the urban area of the village. If there is an official trail that goes into McFarland and crosses the US 51 corridor we'll need to discuss that in our environmental document for the study. Any help is appreciated. Thanks.

Jeff Berens, P.E.

Major Studies Project Manager

WisDOT SW Region - Madison Office

(608) 245-2656



May 14, 2021

Jeff Berens
WisDOT Project Manager
2101 Wright Street
Madison WI 53704

Subject: USH 51 Stoughton to McFarland WisDOT 5845-06-03 Brost Property Impacts Section 4(f)
Coordination

Dear Jeff Berens:

WisDOT has coordinated with the Wisconsin Department of Natural Resources (DNR) and Groundswell Conservancy related to the US 51 Stoughton – McFarland Study. DNR understands that WisDOT has identified Alternative H as WisDOT’s preferred alternative for the study. Since Alternative H was identified, Groundswell Conservancy acquired a property near the intersection of US 51 and Mahoney Road called the Brost Addition, using Knowles-Nelson Stewardship Funds. The property was purchased for conservation and wetland protection purposes and is open to the public for recreational purposes. DNR understands Alternative H will require approximately 1.7 acres of land from the Brost Addition for construction of Alternative H.

In addition to the 1.7 acres, additional easement will be needed for the relocation of the Kegonsa Sanitary District force main currently within an easement on the property. WisDOT intends to purchase the 1.7 acres at fair market value. In addition to the purchase of the property, WisDOT, DNR and Groundswell are working together to determine acceptable mitigation measures to offset stewardship program investment in the Brost Addition.

Total monies expended by WisDOT on mitigation measures will be of equal value to the value of land acquired.

In summary, WisDOT will purchase the approximately 1.7 acres at fair market value and provide mitigation to enhance the Brost Addition of a similar value to the land acquired. Potential mitigation measures discussed to date that are under consideration consist of construction of an improved access and parking area, signage, potential water access enhancements and/or other property enhancements. Mitigation measures will be finalized following real-estate appraisals to determine total value of required mitigation. Appraisal discussions between WisDOT and DNR are in progress.

WisDOT identified that two public comments were received during the public involvement process, WisDOT has provided these comments to DNR and Groundswell for review and consideration prior to sending this letter.

WisDOT has also informed DNR and Groundswell that FHWA may make a finding of de minimis impact related to impacts to the Brost Addition.

DNR believes that the US 51 Study and approximately 1.7 acres of impact to the Brost Addition that would occur from construction of Alternative H as outlined in this letter would not affect the activities, features or attributes that qualify the property for protection under Section 4(f).

If anything in this letter requires further clarification, please contact this office at (608) 228-7928, or email at eric.heggelund@wisconsin.gov.

Sincerely,

Eric Heggelund

Eric Heggelund
Environmental Analysis & Review Specialist

cc: Joel Brown, WisDOT
Pam Foster Felt, DNR
Jim Welsh, Groundswell



May 19, 2021

Jeff Berens, P.E.
Major Studies Project Manager
WisDOT SW Region - Madison Office

Dear Jeff:

WisDOT has coordinated with the Wisconsin Department of Natural Resources (DNR) and Groundswell Conservancy related to the US 51 Stoughton – McFarland Study. DNR and Groundswell understand that WisDOT has identified Alternative H as WisDOT's preferred alternative for the study. Since Alternative H was identified, Groundswell Conservancy acquired a property near the intersection of US 51 and Mahoney Road called the Brost Addition, using Knowles-Nelson Stewardship Funds. The property was purchased for conservation and wetland protection purposes and is open to the public for recreational purposes. DNR and Groundswell understand Alternative H will require approximately 1.7 acres of land from the Brost Addition for construction of Alternative H. An easement has been accepted and recorded with Dane County restricting uses outlined in the Knowles-Nelson Stewardship Grant Agreement.

In addition to the 1.7 acres, additional easement will be needed for the relocation of the Kegonsa Sanitary District force main currently within an easement on the property. WisDOT intends to purchase the 1.7 acres at fair market value. In addition to the purchase of the property, WisDOT, DNR and Groundswell are working together to determine acceptable mitigation measures to offset stewardship program investment in the Brost Addition.

Total monies expended by WisDOT on mitigation measures will be of equal value to the value of land acquired.

In summary, WisDOT will purchase the approximately 1.7 acres at fair market value and provide mitigation to enhance the Brost Addition of a similar value to the land acquired. Potential mitigation measures discussed to date that are under consideration consist of construction of an improved access and parking area, signage, potential water access enhancements and/or other property enhancements. Mitigation measures will be finalized following real-estate appraisals to determine total value of required mitigation. Appraisal discussions between WisDOT and DNR are in progress.



WisDOT identified that two public comments were received during the public involvement process, WisDOT has provided these comments to DNR and Groundswell for review and consideration prior to sending this letter.

WisDOT has also informed DNR and Groundswell that FHWA may make a finding of de minimis impact related to impacts to the Brost Addition.

Groundswell believes that the US 51 Study and approximately 1.7 acres of impact to the Brost Addition that would occur from construction of Alternative H as outlined in this letter would not affect the activities, features or attributes that qualify the property for protection under Section 4(f).

Sincerely,



Jim Welsh
Executive Director

Z:\Stewardship\Projects\Lower Mud Lake\Brost\Stewardship\DOT Taking\Brost Addition Response to DOT.docx



IN REPLY REFER TO:

United States Department of the Interior

OFFICE OF THE SECRETARY
Office of Environmental Policy and Compliance
Custom House, Room 244
200 Chestnut Street
Philadelphia, Pennsylvania 19106-2904

July 1, 2020

9043.1
ER 20/0206

Bethaney Bacher-Gresock
U.S. Department of Transportation
Wisconsin Division Office
525 Junction Road, Suite 8000
Madison, WI 53717

Dear Ms. Bacher-Gresock:

The Department of the Interior (Department) has reviewed the Draft Section 4(f) evaluation for the US 51 Corridor Study, from I-39/90 to US 12/18, in Dane County, Wisconsin (the project). The document considers effects under Section 4(f) of the Department of Transportation Act of 1966 (codified at 49 U.S.C. 303) associated with the project. The project sponsors are the Wisconsin Department of Transportation (WisDOT) and the Federal Highway Administration (FHWA).

Background Information

The US 51 corridor is an important commuter route in southeastern Dane County, connecting I-39/90 and US 12/18 which are both National Highway System routes and *Connections 2030* Backbone routes. The purpose of the project is to provide a safe and efficient transportation system in the US 51 corridor to serve present and long-term travel demand while minimizing disturbance to the environment. This will be obtained by working to address existing safety conditions, accommodate travel demand, address existing pavement conditions, improve bicycle and pedestrian accommodations, and considering corridor preservation and long-term planning measures.

Section 4(f) Comments

Section 4(f) applies to both historic and recreational resources impacted by the project.

There are several Section 4(f) resources in the study area:

- Historic Maple Grove School
- Historic Districts in Stoughton (Five Historic Districts)
- Barber Campsite (archaeological site)

- Lincoln Point Park (recreational area)
- Colladay Point Park (recreational area)
- Colladay Mound (archaeological site)
- Historic Olson-Hemsing Farmstead (12 historic resources)
- Bird Effigy (archaeological site)
- Babcock Park (recreational area)
- Babcock Park Archaeological Site
- Lower Yahara River Trail (recreational area)
- Brost Addition to Mud Lake (recreational area)

Project sponsors evaluated multiple alternatives as part of this project. The preferred alternative (Alternative H) would have Section 4(f) impacts on Babcock Park. Proposed actions for transportation improvements in the vicinity of Babcock Park would include road widening, adding turning lanes, relocating vehicle entryways, adding pedestrian routes, and improving pedestrian crossings. These actions would result in acquisition and use of estimated 0.5 acres for rights of way and approximately 2.9 acres of temporary limited easement.

Impacts to Babcock Park cannot be avoided under the preferred alternative because park land is located on both sides of US 51, and Alternative H widens the roadway in this area and requires real estate acquisition. Other alternatives would result in extensive impacts to other resources protected by Section 4(f). WisDOT and FHWA have met with Dane County Parks and the Park Commission on several occasions to discuss potential impacts to Babcock Park and propose mitigation measures. FHWA has proposed to coordinate all work with Dane County Parks.

The FHWA has determined that no other properties would have Section 4(f) use determinations, including the Barber Campsite archeological site, which would be excavated and completely removed under the preferred alternative. Section 4(f) does not apply to that site if FHWA determines that the archeological resource is important chiefly because of what can be learned by data recovery and has minimal value for preservation in place, and the State Historic Preservation Office (SHPO), Tribal Historic Preservation Office (THPO), and Advisory Council on Historic Preservation (ACHP)(if participating) does not object to this determination. The draft evaluation does not describe how this determination of "minimal value for preservation in place" was made, and does not include the required THPO, SHPO, and ACHP concurrence with the determination. It is anticipated that further consultation will result in the development of a memorandum of agreement, documenting concurrence and mitigation measures for the project.

The FHWA has preliminarily determined there is no feasible and prudent avoidance alternative to the use of land from the Section 4(f) property. Furthermore, the preferred alternative includes all possible planning to minimize harm to the Section 4(f) resource resulting from such use. FHWA will finalize this Section 4(f) evaluation after considering public comments or testimony received at the project's upcoming public hearing, coordination with the US Department of Interior and officials with jurisdiction, and a legal sufficiency review.

The Department concurs with the draft determination that the project's preferred alternative constitutes a use under Section 4(f). The Department also concurs that there is no feasible or prudent alternative that would meet the purpose and need of the project and avoid the use and

impact of the Section 4(f) properties. The Department has no objection to the Draft Section 4(f) evaluation, and expects that the final determination will include all required concurrences, agreements, and mitigation measures developed through consultation with the SHPO, THPO, and ACHP.

The Department has a continuing interest in working with the FHWA and WisDOT to ensure impacts to resources of concern are adequately addressed. For issues concerning Section 4(f) resources, please contact Tokey Boswell, Chief, Planning and Compliance Division, Regions 3, 4, and 5, National Park Service, 601 Riverfront Drive, Omaha, Nebraska 68102, or by email at tokey_boswell@nps.gov.

We appreciate the opportunity to provide these comments.

Sincerely,

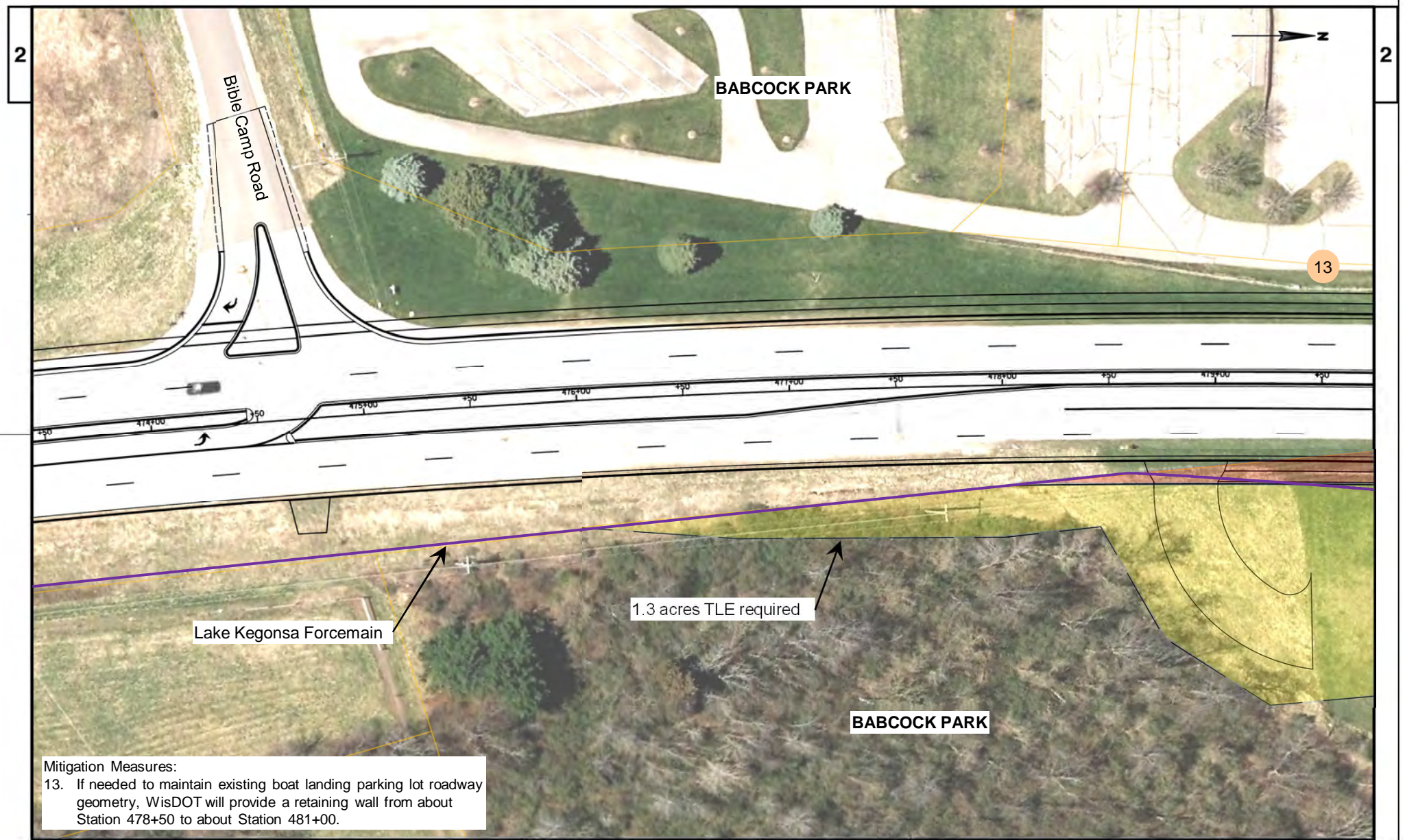
**JOHN
NELSON**

Digitally signed by
JOHN NELSON
Date: 2020.06.30
13:21:31 -04'00'

John Nelson
Acting Regional Environmental Officer
Office of Environmental Policy and Compliance

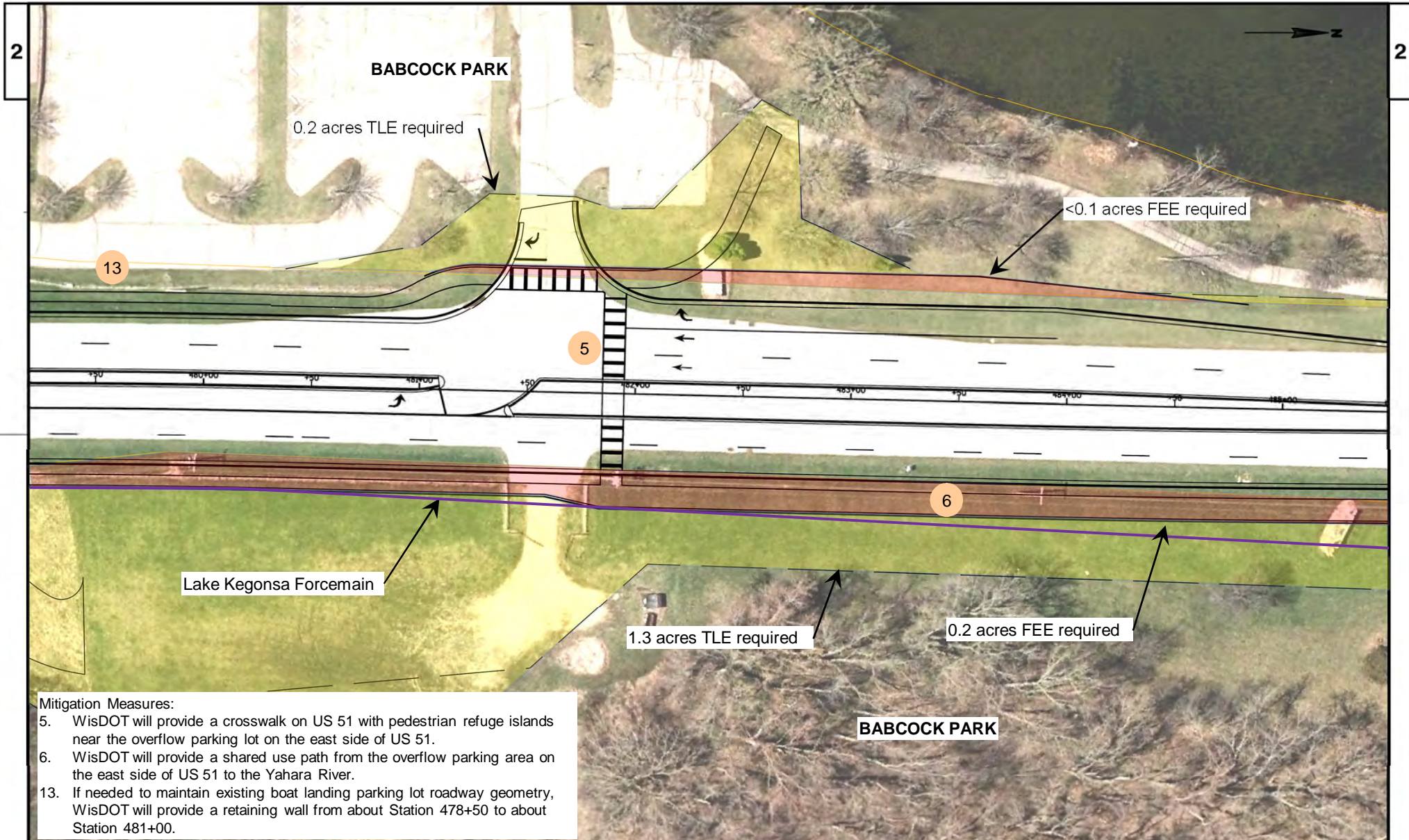
**APPENDIX B
PRELIMINARY PLAN SHEETS**

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PROJECT NO: 5845-06-03	HWY: US 51	COUNTY: DANE	BABCOCK PARK - FACTOR SHEET B-8	SHEET	E
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FILE NAME : S:\MAD\1000-1099\1089\47\Designs\5\101es\Reports\EA_Graphics\Factor_Sheets\Babcock Park_Plan_View_1.dgn PLOT DATE : 10/3/2013 PLOT BY : jusermore PLOT NAME : PLOT SCALE : ##,....,plotscale,....## WISDOT/CADD SHEET 42



PROJECT NO: 5845-06-03

HWY: US 51

COUNTY: DANE

BABCOCK PARK - FACTOR SHEET B-8

SHEET

E

FILE NAME : S:\MAD\1005-1099\1089\347\Drawings\Structures\Reports\EA\Graphics\Factor Sheets\Babcock Park\Plan View 2.dgn

PLOT DATE : 10/3/2019

PLOT BY : lukemoms

PLOT NAME :

PLOT SCALE : #1.....plotscale.....** WISDOT/CADD SHEET 42

2

Mitigation Measures:

- 3. WisDOT will include relocation/recalibration of the USGS station at Babcock Park.
- 4. WisDOT will provide an access path from proposed US 51 path south of the Yahara River bridge to the existing fishing pier and dam.
- 8. WisDOT will provide a connector path from the proposed US 51 path north of the Yahara River bridge to the parking lot and existing park path on the west side of US 51.
- 9. WisDOT will reconstruct the lock parking lot as single loaded on the west side and expand the lot north to the existing storage sheds.
- 12. WisDOT will construct a retaining wall from Station 489+00 to Station 494+00 that includes a transition ramp to provide access to the parking lot.

0.2 acres TLE required
 <0.1 acres FEE required

BABCOCK PARK

1.2 acres TLE required

0.2 acres FEE required

9

BABCOCK PARK

3

4

7

12

8

BABCOCK PARK

11

7

Lake Kegonsa Forcemain

0.2 acres FEE required

1.3 acres TLE required

6

BABCOCK PARK

Mitigation Measures:

- 6. WisDOT will provide a shared use path from the overflow parking area on the east side of US 51 to the Yahara River.
- 7. WisDOT will provide sidewalk on the east side of the Yahara River Bridge and a bicycle/pedestrian path on west side of the bridge.
- 11. WisDOT will lengthen the span of the Yahara River Bridge to be at least the same as the existing dam structure opening.

PROJECT NO: 5845-06-03

HWY: US 51

COUNTY: DANE

BABCOCK PARK - FACTOR SHEET B-8

SHEET

E

FILE NAME : S:\MAD\1000--1099\1089\367\Drawings-Studies-Reports\EA\Graphics\Factor Sheets\Babcock Park Plot View 3.dgn

PLOT DATE : 10/15/2013

PLOT BY : j...@wisconsin.gov

PLOT NAME :

PLOT SCALE : 1/8" = 1'-0" LOTS OF SCALE... WISDOT/CAODS SHEET 42

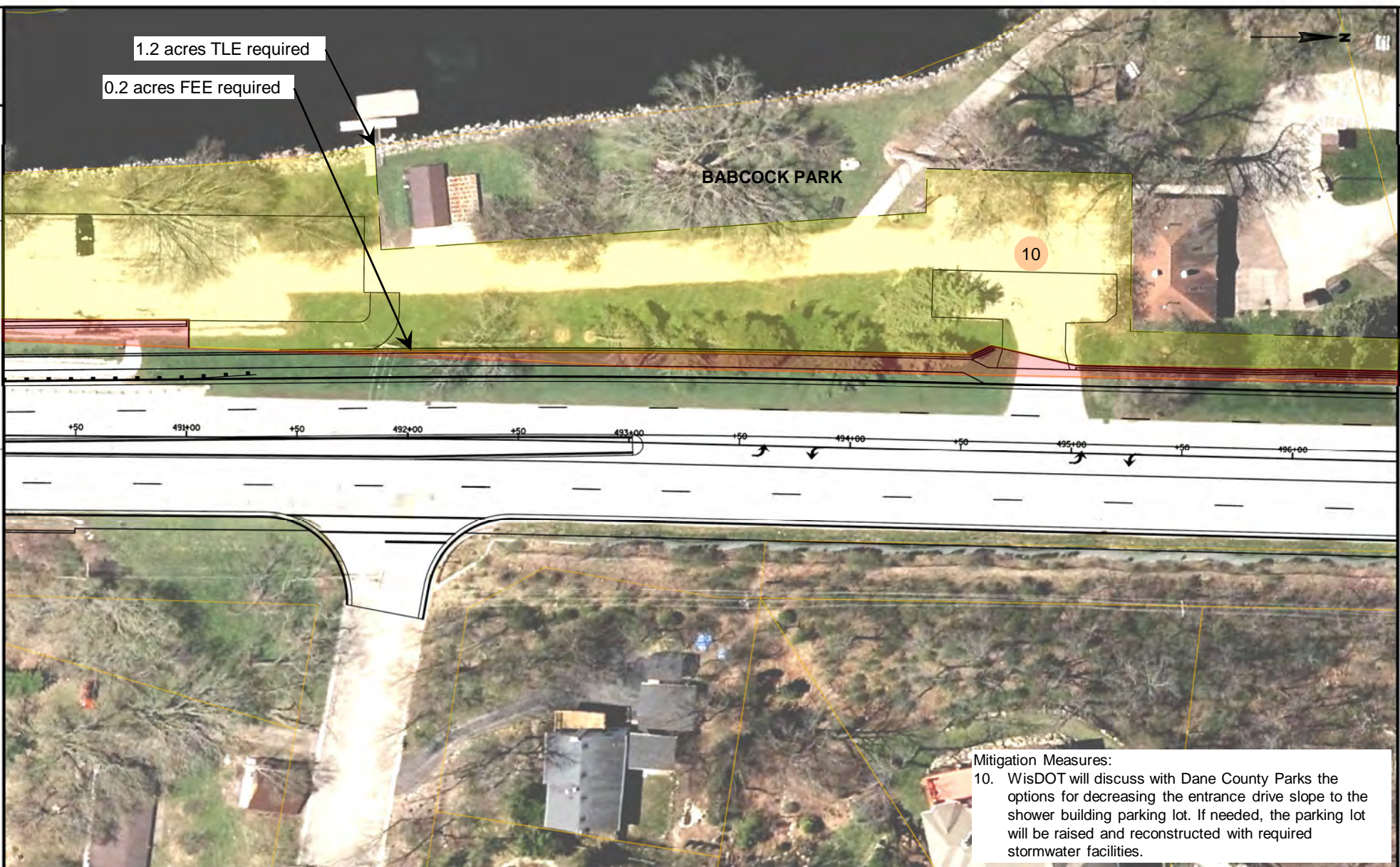
2

2

1.2 acres TLE required
0.2 acres FEE required

BABCOCK PARK

10



Mitigation Measures:
 10. WisDOT will discuss with Dane County Parks the options for decreasing the entrance drive slope to the shower building parking lot. If needed, the parking lot will be raised and reconstructed with required stormwater facilities.

PROJECT NO: 5845-06-03 HWY: US 51 COUNTY: DANE BABCOCK PARK - FACTOR SHEET B-8 SHEET E

FILE NAME : S:\MAD\1000--1099\1089\367\Drawings-Studies-Reports\EA\Graphics\Factor Sheets\Babcock Park Plot View.dwg PLOT DATE : 10/31/2013 PLOT BY : j.unwimmo... PLOT NAME : PLOT SCALE : 1/8"=1'-0" WISDOT/CAOS SHEET 42

2

2



1.2 acres TLE required

0.2 acres FEE required

BABCOCK PARK

14

15

Mitigation Measures:

- 14. WisDOT will provide a screening and/or barrier wall adjacent to the campground. Between the wall and US 51 west curb line, sidewalk will be provided.
- 15. WisDOT is willing to provide some aesthetic and informational provisions on the screening and/or barrier wall and will coordinate these items with Dane County Parks.

PROJECT NO: 5845-06-03

HWY: US 51

COUNTY: DANE

BABCOCK PARK - FACTOR SHEET B-8

SHEET

E

FILE NAME : S:\MAD\1000--1099\1089\947\Designs-Studies-Reports\EA Graphics\Factor Sheets\Babcock Park Plan View 5.dgn

PLOT DATE : 10/3/2019

PLOT BY : _username_

PLOT NAME :

PLOT SCALE : **.....plotscale.....** WISDOT/CAODS SHEET 42

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**APPENDIX C
ALTERNATIVES COMPARISON MATRIX**

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BASIC SHEET 6--ALTERNATIVES COMPARISON MATRIX

All estimates including costs are based on conditions described in this document at the time of preparation in the year of expenditure (YOE). Additional agency or public involvement may change these estimates in the future.

PROJECT PARAMETERS	Unit of Measure	ALTERNATIVES			
		No Build ¹	A	B	H
Project Length	Miles	--	17.7	17.7	17.7
PRELIMINARY COST ESTIMATE ²		2016 (FY)	2016 (FY)	2016 (FY)	2020 (FY)
Construction	Million \$	28	97	294 to 306	166.6
Real Estate	Million \$	0	2	10 to 15	7.5
TOTAL	Million \$	28	99	304 to 321	174.1
TOTAL (YOE)	Million \$	--	--	--	203.4
LAND CONVERSIONS					
Total Area Converted to ROW	Acres	0	59	272 to 299	66
REAL ESTATE					
Number of Farms Affected	Number	0	37	159	37
Total Area Required from Farm Operations	Acres	0	34.1	183 to 223	45.7
AIS Required		<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No
Farmland Rating	Score	--	172	197	169
Total Buildings Required	Number	0	1	18 to 26	2
Housing Units Required	Number	0	1	14 to 20	2
Commercial Units Required	Number	0	0	2	0
Other Buildings or Structures Required	Number & Type	0	0	2 to 4 Barns and Community Facilities	0
ENVIRONMENTAL FACTORS					
Indirect Effects		<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No
Cumulative Effects		<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No
Environmental Justice Populations		<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No
National Register Eligible Historic Structures in the Area of Potential Effect	Number	0	2 Sites 5 Historic Districts	4 Sites 5 Historic Districts	2 Sites 5 Historic Districts
National Register Eligible Archeological Sites in the Area of Potential Effect	Number	0	5	6	5
Burial Site Protection (authorization required)		<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No
106 MOA Required		<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No
Section 4(f) Evaluation Required		<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No
Section 6(f) Land Conversion Required		<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No
Floodplain		<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No
Unique Upland Habitat Identified		<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No
Total Wetlands Filled	Acres	0	8.2	8.4 to 9.4	8.4
Stream Crossings	Number	0	6	7	6
Threatened/Endangered Species		<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No
Noise Analysis Required		<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No
Receptors Impacted	Number			69 to 78 ³	38
Contaminated Sites	Number	0	25	36 to 40	76

¹ The estimated cost of routine maintenance through the design year should be included in the "Construction" box for the No Build alternative.
² Only fiscal year 2016 costs were estimated for the No Build Alternative, Alternative A and Alternative B. A cost risk analysis was completed to arrive at an estimated year of expenditure (YOE) project cost for the preferred alternative (Alternative H).
³ The noise analysis for Alternative B was completed prior to dismissal of the alternative. The traffic forecast used at that time had higher traffic volumes than the current traffic forecast and the analysis represents a worst case scenario for Alternative B.

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APPENDIX D
EXCERPT FROM DOCUMENTATION FOR CONSULTATION

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12. Provide copies or summaries of the views provided to date by any consulting parties, Indian tribes or Native Hawai'ian organizations, or the public, including any correspondence from the SHPO and/or THPO.

The Wisconsin SHPO has concurred with the findings of the identification and evaluation surveys for both archaeology and architecture/history.

No comments have been received from consulting parties, Indian tribes or Native Hawai'ian organizations, and/or the public.

III. Optional Information

13. Please indicate the status of any consultation that has occurred to date. Are there any consulting parties involved other than the SHPO/THPO? Are there any outstanding or unresolved concerns or issues that the ACHP should know about in deciding whether to participate in consultation?

Public involvement for the US 51 Corridor Study has been ongoing since 2005 when the initial Alternatives Assessment Workshop was held following the Needs Assessment. Following that workshop, public involvement was related to the development of alternatives for the Environmental Impact Statement being prepared. After 2014, public involvement was related to obtaining input on near-term improvements and providing an overview of the tiered process for long-term corridor improvements.

A Public Information Meeting (PIM) was held on August 26, 2015 to solicit public input regarding the project and all four alternatives (Attachment 19). Prior to this meeting, newsletters and postcard invitations were sent to all property owners and businesses within an approximate one-half-mile distance of the project corridor as well as all City of Stoughton and Village of McFarland residents and members of the policy advisory committee (agencies, tribes, and local governments). The project scope and goals were presented along with the new Two- and Four-Lane Reconstruction and Resurfacing with Intersection Improvements Alternative¹. Attendees were given the opportunity to ask questions and express concerns about the project vocally or in written form on comment sheets. Fifty-nine written comments and several additional verbal comments were received and recorded. Based on recorded public comments, the Preferred Alternative received the most support, followed by the Low Build Alternative, with the Four-Lane Expansion Alternative receiving the least support. No attendees expressed concern directly related to any historic properties. One attendee expressed the opinion that the trees along East Main Street should be retained, though the East Main Street Historic District was not mentioned.

Between June of 2008 and December of 2012, consultants from Great Lakes Archaeological Research Center, Inc. (GLARC) attempted to contact the Dane County Historical Society via multiple phone and email messages to notify them of the project activity and solicit any input or concerns (Attachment 20). No response was received and messages were not returned. In September of 2013, GLARC also contacted the Stoughton Landmarks Commission and the Stoughton Historical Society via email. The Stoughton Landmarks Commission responded with questions regarding specific project activities and architecture/history survey methodology. GLARC staff responded to the Commission's questions via email and provided the group with contact information for Strand Associates for follow up on specific

¹ An earlier PIM was held in October 2012 at which time information about the No Build, Low Build, and Four-Lane Expansion Alternatives was presented. Subsequently, it was determined that the US 51 corridor alternatives proposed in the DEIS would not receive funding for the next major action to advance the project.

project activities. The group did not identify any additional historic resources in the project area. The Stoughton Historical Society did not respond with any questions or concerns and did not identify any additional resources in the project area.

A consultation meeting was held on February 17, 2016 (Attachment 21). The impacts from the project to NRHP-eligible archaeological sites were discussed. Data recovery of a portion of each site was agreed upon as the appropriate mitigation measure.

There are no outstanding or unresolved concerns or issues.

14. Does your agency have a website or website link where the interested public can find out about this project and/or provide comments? Please provide relevant links:

Yes. <http://wisconsindot.gov/Pages/projects/by-region/sw/5139901218/default.aspx>

15. Is this undertaking considered a “major” or “covered” project listed on the Federal Infrastructure Projects Permitting Dashboard or other federal interagency project tracking system? If so, please provide the link or reference number:

No.

The following are attached to this form (check all that apply):

- Section 106 consultation correspondence
- Maps, photographs, drawings, and/or plans
- Additional historic property information
- Other (See Attachments List)

APPENDIX E
***DE MINIMIS* FINDING FOR BROST ADDITION TO MUD LAKE**

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Wisconsin Federal Highway Administration
Finding of *De Minimis* Impact on Parks, Recreation Areas and Wildlife and Waterfowl
Refuges (Updated 7/25/2017)

1. Project Description

WISDOT ID: 5845-06-03

Route: US 51

Termini: I-39/90 to US 12/18 (Madison South Beltline)

City/County: City of Stoughton and Village of McFarland, Dane County

Project Description:

The study area for the United States Highway (US) 51 Corridor Study is located in south central Wisconsin in the southeast corner of Dane County. The area lies directly southeast of the city of Madison. The US 51 study corridor extends between the logical termini of Interstate 39/90 (I-39/90), located east of the city of Stoughton, and US 12/18 (Madison South Beltline) in the city of Madison, a distance of 18.6 miles. US 51 connects I-39/90 and US 12/18, which are both National Highway System (NHS) routes and Connections 2030 Backbone routes. NHS routes are important to the nation's economy, mobility and defense. Connections 2030 Backbone (and Connector) routes are identified in Wisconsin's Connections 2030 Long-Range Multimodal Transportation Plan adopted October 2009 and signify Wisconsin's most important highways. While US 51 is not a NHS route, Backbone or Connector route, the US 51 study corridor is an important commuter route in southeastern Dane County that connects I-39/90 and US 12/18. US 51 functions as a principal arterial for most of the corridor except for the 5.7-mile section east of Stoughton from I-39/90 to County N, which is classified as a minor arterial.

The purpose of the project is to provide a safe and efficient transportation system in the US 51 corridor to serve present and long-term travel demand while minimizing disturbance to the environment. This will be obtained by working to address existing safety conditions, accommodate travel demand, addressing existing pavement conditions, improving bicycle and pedestrian accommodations and considering corridor preservation and long-term planning measures. An Environmental Assessment (EA) will be completed for the project to satisfy National Environmental Policy Act (NEPA) requirements. In addition to the No Build Alternative (No Action), Alternative A (Low Build), Alternative B (4-Lane Expansion), and Alternative H (Hybrid) were evaluated in the EA. Alternative H is an alternative that combines features of Alternatives A and B and WisDOT identified Alternative H as the preferred alternative. Each of the build alternatives have a total length of 17.7 miles compared to the 18.6-mile length of the corridor study limits.

Alternative H has six main components that would include the following:

1. Reconstruction of existing rural 2-lane US 51 east of Stoughton.
2. Reconstruction of existing urban 2- and 4-lane US 51 through Stoughton.
3. Capacity expansion from a 2-lane section to a 4-lane urban section along west side of Stoughton.
4. Reconstruction of existing rural 2-lane US 51 (Stoughton to McFarland) with intersection improvements.
5. Reconstruction of existing urban 4-lane in McFarland.
6. Pavement replacement of existing rural expressway between Larson Beach Road and Terminal Drive/Voges Road in McFarland, with Siggelkow Road interchange ramp

improvements and addition of an auxiliary lane in each direction north of Siggelkow Road to Terminal Drive/Voges Road.

A cost risk analysis was completed to arrive at an estimated project cost. The year of expenditure (YOE) cost is a total cost for the entire project timeline from the completion of the environmental document to the anticipated completion of construction by the end of 2029. The risk-adjusted cost results for the project show that 70 percent of the time, total project costs will be \$203.4 million or below at year of expenditure (YOE). In 2020 dollars, this equates to a 70th percentile total project cost of \$174.1 million.

2. Name of Section 4(f) resource: (If the resource is a park and a historic property please indicate the historic property name and the park name if different.)

Brost Addition to Mud Lake (Brost Addition)

3. Description of Section 4(f) resource (Include a map and/or photos of the property in relation to the proposed project):

The property is located along the east and west sides of US 51 near Mahoney Road, see Figure 1. The Brost Addition is approximately 68 acres of public land owned and operated by the Groundswell Conservancy. The property was acquired in part with a grant from WDNR and is open to the public with use defined in the Draft Land Management Plan and grant document as:

1. for conservation and recreation purposes (Management Plan).
2. to protect, enhance and restore wildlife habitat and natural communities (Project Purpose in the grant document).
3. to enhance opportunities for wildlife-based outdoor recreation (Project Purpose in the grant document).

These attributes of the Brost Addition property make it eligible for protection under Section 4(f).

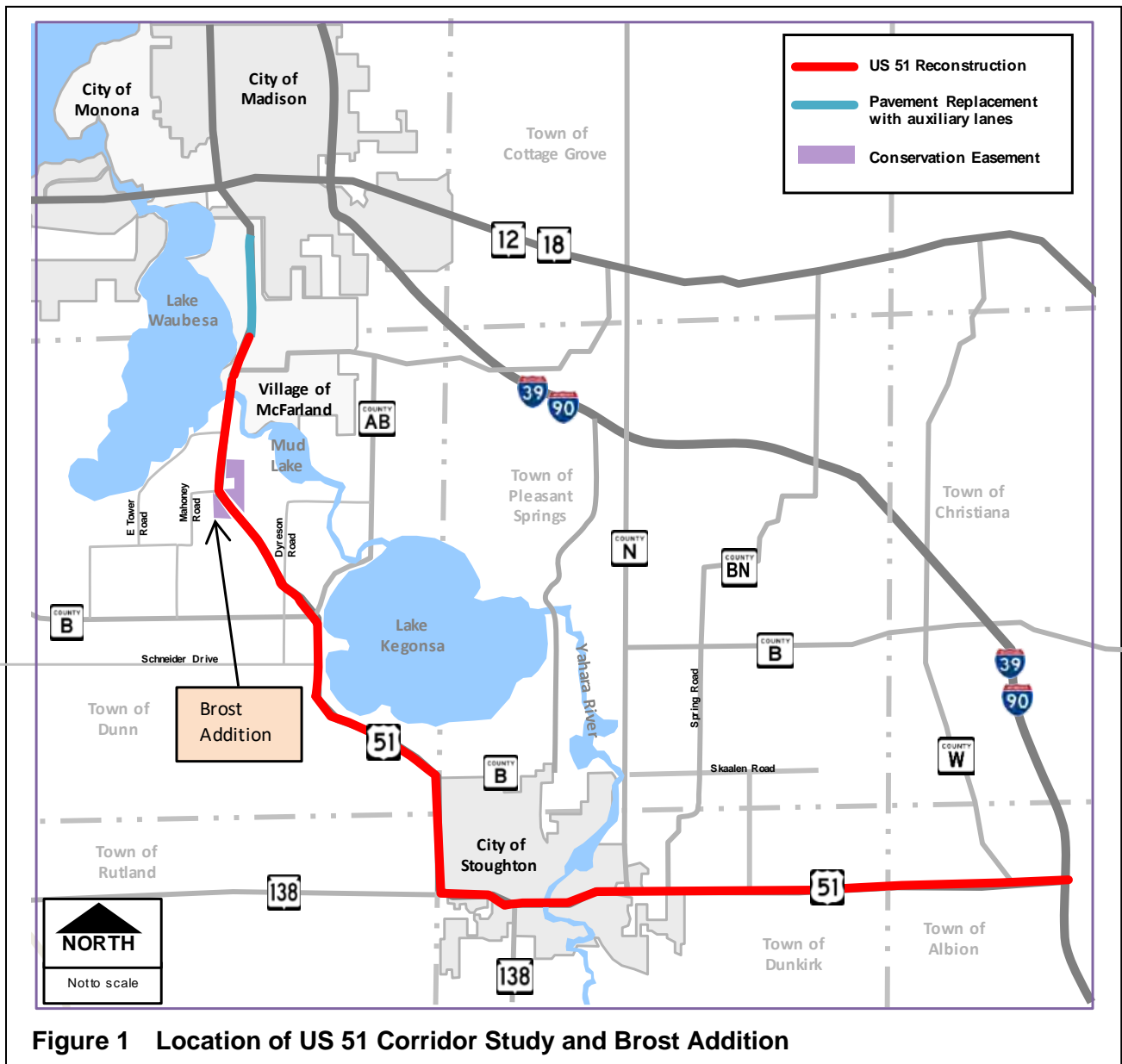


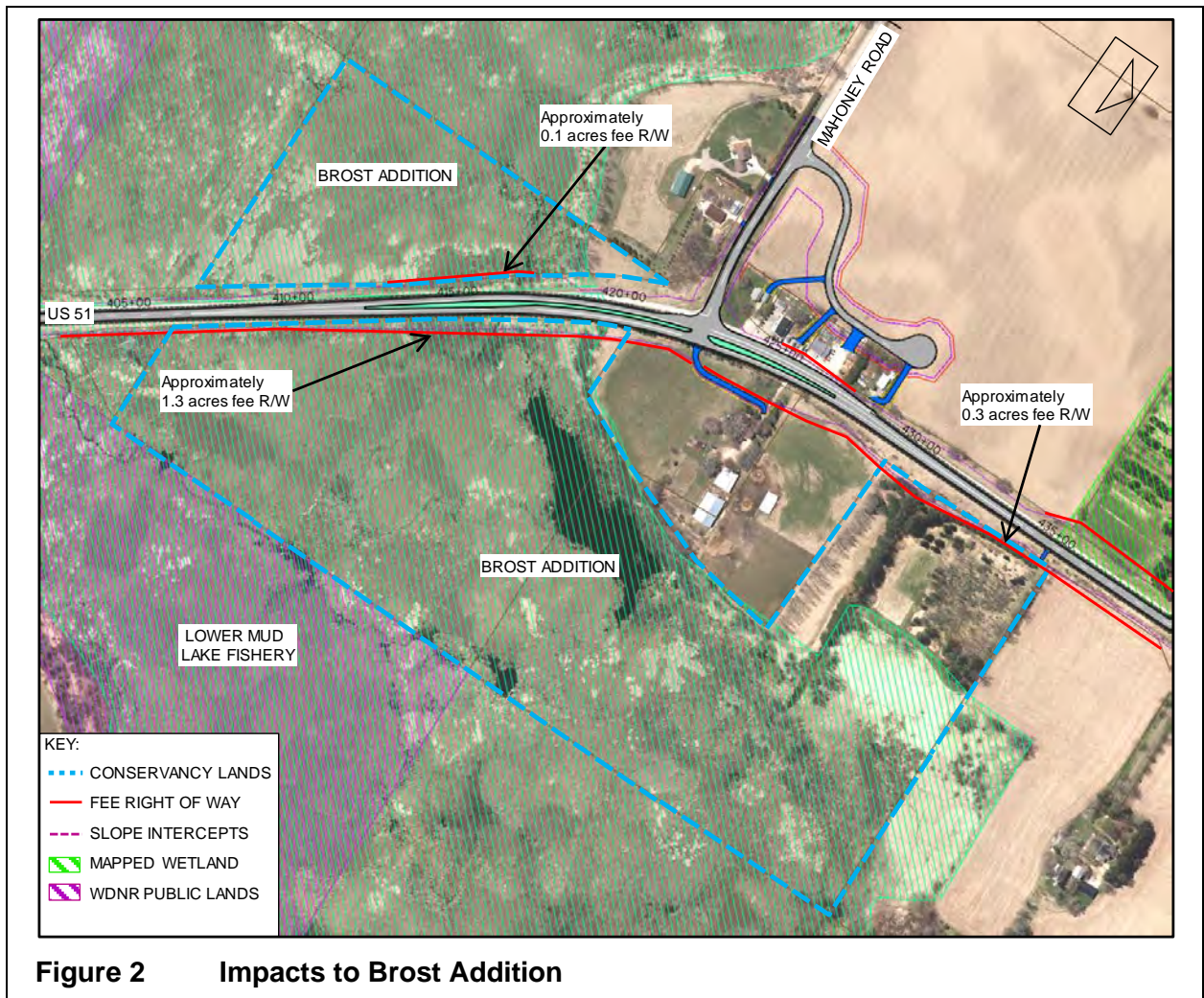
Figure 1 Location of US 51 Corridor Study and Brost Addition

4. Description of impacts:

The proposed action would require approximately 1.7 acres of fee right of way from the Brost Addition. The impacts would be related to the proposed reconstruction of the US 51 pavement structure (pavement, aggregate and subbase layers) and repair and replacement of ditches and culverts along the Brost Addition property. The existing US 51 section has two 12-foot travel lanes with 8-foot shoulders (3-foot paved). The proposed section will provide two 12-foot travel lanes with 10-foot shoulders (6-foot paved). In the vicinity of the Mahoney Road intersection, design standards require a median be provided to both the north and south approaches to allow development of a left-turn lane for northbound, left-turning traffic. A southbound, right turn lane is also proposed at Mahoney Road. The turn lanes and medians require the alignment to be shifted slightly east to avoid multiple relocation impacts to several businesses on the west side of US 51. This results in additional right of way required on the east side of US 51. Thus, the impacts to the Brost Addition result from the

pavement reconstruction and reconstruction of the Mahoney Road intersection where turn lanes and medians will be added.

Fee right of way impacts (1.7 acres total) are anticipated along both sides of US 51 (see Figure 2). In addition to these highway reconstruction impacts, the Kegonsa Sanitary District (KSD) maintains a sanitary sewer force main along the east side of US 51 within an easement on the property. KSD has indicated it will relocate portions of the force main as a result of the US 51 improvements and the need for additional easement acquisition by the Sanitary District is anticipated.



5. Discuss avoidance, minimization, and compensation efforts and how the impacts after avoidance, minimization, and compensation do not adversely affect the activities, features, and attributes listed in Number 3 above:

The proposed action will not result in adverse effects on the activities, features, and attributes of the Brost Addition property.

To minimize impacts through wetland areas, which is most of the Brost Addition property, the existing roadway grade of 4 percent will be maintained. The roadway profile in this area would normally require a 3 percent grade from the high points to low point in the wetland to

meet current design standards. Using a 4 percent grade will maintain the existing profile, minimize fill through the floodplain, and reduce impacts by approximately 0.3 to 0.5 acres. Preliminary approval by FHWA and WisDOT was obtained in February 2016 for this exception to design standards. During final roadway design, the project team will seek final approval for a design exception for this grade.

Near the northern end of the Lower Mud wetland complex, a tributary to Keenans Creek crosses US 51 within the Brost Addition property. The tributary flows beneath US 51 through a 54-inch concrete pipe. The proposed action includes evaluation of the existing culvert size during final design and culvert replacement during reconstruction of US 51. The re-sized culvert will potentially benefit the Brost Addition property habitat by improving the hydraulic connection between the wetlands to the west and east of US 51. The reconstructed US 51 facility, with wider paved shoulders, will also benefit motorists and bicyclists using US 51, including those traveling to the Brost Addition property.

The property owner will be compensated monetarily for fee acquisition by WisDOT and for the terms of easements potentially required for the KSD force main by KSD. Improvements and restoration for the roadway will be completed as part of the proposed action. Disturbed areas will be restored.

Coordination with the WDNR and Groundswell Conservancy is ongoing and potential mitigation measures for the anticipated impacts to the Brost Addition are being evaluated.

6. Describe the public involvement process and results:

The public has been afforded the opportunity to review and comment on the effects of the proposed action on the protected activities, features, and attributes of the Section 4(f) resource at the October 2020 public involvement meeting (PIM). Two comments were received about the Section 4(f) property during the PIM. One comment agreed with minimizing and mitigating the impacts to the property and one requested that WisDOT be respectful of the conservation land. A public hearing was held for the study in April 2021. No comments or testimony were provided related to the Brost Addition.

7. Name of and notification to the official(s) with jurisdiction over the property:

The officials with jurisdiction over the Section 4(f) resource are WDNR and the Groundswell Conservancy. Officials were notified that FHWA may make a de minimis finding under Section 4(f). Letters from WDNR and the Groundswell Conservancy acknowledging the project will not affect the activities, features or attributes that make the property eligible for protection under Section 4(f) are provided in Appendix A of the Section 4(f) Evaluation, pages A-35 through A-38.

8. Describe the results of coordination with the official(s) with jurisdiction over the property following public involvement (attach correspondence from the official(s)):

Coordination with WDNR and Groundswell Conservancy is ongoing and potential mitigation measures discussed to date that are under consideration consist of construction of an improved access and parking area, signage, potential water access enhancements and/or other property enhancements. Mitigation measures will be finalized following real-estate appraisals to determine total value of required mitigation.

