



# Wisconsin Department of Transportation

July 2, 2018

**Division of Transportation Systems Development**  
Bureau of Project Development  
4822 Madison Yards Way, 4<sup>th</sup> Floor South  
Madison, WI 53705

Telephone: (608) 266-1631  
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## NOTICE TO ALL CONTRACTORS:

**Proposal #10: 2788-00-71, WISC 2017 512**  
**Waukesha Bypass**  
**Genesee Rd to Fiddlers Creek Dr**  
**USH 018**  
**Waukesha County**

## Letting of July 10, 2018

This is Addendum No. 01, which provides for the following:

### Special Provisions:

| <b>Revised Special Provisions</b> |                                                                            |
|-----------------------------------|----------------------------------------------------------------------------|
| Article No.                       | Description                                                                |
| 5                                 | Prosecution and Progress                                                   |
| 6                                 | Traffic                                                                    |
| 8                                 | Utilities                                                                  |
| 11                                | Other Contracts                                                            |
| 12                                | Railroad Insurance and Coordination – Wisconsin and Southern Railroad LLC. |

| <b>Added Special Provisions</b> |                                                                                          |
|---------------------------------|------------------------------------------------------------------------------------------|
| Article No.                     | Description                                                                              |
| 139                             | Seismograph                                                                              |
| 140                             | Wall R-67-145 Fence Chain Link Polymer Coated 4-FT; Fence Chain Link Polymer Coated 4-FT |
| 141                             | Temporary Precast Trench Drain                                                           |

**Schedule of Items:**

| <b>Revised Bid Item Quantities</b> |                                              |      |              |                  |                |
|------------------------------------|----------------------------------------------|------|--------------|------------------|----------------|
| Bid Item                           | Item Description                             | Unit | Old Quantity | Revised Quantity | Proposal Total |
| 204.0150                           | Removing Curb & Gutter                       | LF   | 7,210        | 1,230            | 8,440          |
| 601.0409                           | Concrete Curb & Gutter 30-Inch Type A        | LF   | 1,965        | 1,230            | 3,195          |
| 611.0530                           | Manhole Covers Type J                        | EACH | 17           | 1                | 18             |
| 611.2004                           | Manholes 4-FT Diameter                       | EACH | 23           | 1                | 24             |
| 643.0300                           | Traffic Control Drums                        | DAYS | 197,000      | 4,000            | 201,000        |
| 643.0410                           | Traffic Control Barricades Type II           | DAYS | 1,900        | 100              | 2,000          |
| 643.0420                           | Traffic Control Barricades Type III          | DAYS | 34,500       | 2,500            | 37,000         |
| 643.0705                           | Traffic Control Warning Lights Type A        | DAYS | 56,400       | 3,100            | 59,500         |
| 643.0715                           | Traffic Control Warning Lights Type C        | DAYS | 30,800       | 1,100            | 31,900         |
| 643.0800                           | Traffic Control Arrow Boards                 | DAYS | 1,300        | 80               | 1,380          |
| 643.0900                           | Traffic Control Signs                        | DAYS | 163,810      | 12,190           | 176,000        |
| 652.0225                           | Conduit Rigid Nonmetallic Schedule 40 2-Inch | LF   | 8,041        | 633              | 8,674          |
| SPV.0090.02                        | Fence Chain Link Polymer Coated 4-FT         | LF   | 540          | -181             | 359            |
| SPV.0090.03                        | Fence Chain Link Polymer Coated 6-FT         | LF   | 1,947        | -1,139           | 808            |

| <b>Added Bid Item Quantities</b> |                                                    |      |              |                  |                |
|----------------------------------|----------------------------------------------------|------|--------------|------------------|----------------|
| Bid Item                         | Item Description                                   | Unit | Old Quantity | Revised Quantity | Proposal Total |
| 205.0200                         | Excavation Rock                                    | CY   | 0            | 110              | 110            |
| 608.0005                         | Storm Sewer Rock Excavation                        | CY   | 0            | 120              | 120            |
| 652.0125                         | Conduit Rigid Metallic 2-Inch                      | LF   | 0            | 60               | 60             |
| 653.0220                         | Junction Boxes 18x6x6-Inch                         | EACH | 0            | 6                | 6              |
| 999.1000.S                       | Seismograph                                        | LS   | 0            | 1                | 1              |
| SPV.0090.06                      | Wall R-67-145 Fence Chain Link Polymer Coated 4-FT | LF   | 0            | 228              | 228            |
| SPV.0090.07                      | Fence Chain Link Polymer Coated 4-FT               | LF   | 0            | 1,092            | 1,092          |
| SPV.0090.08                      | Temporary Precast Trench Drain                     | LF   | 0            | 230              | 230            |

**Plan Sheets:**

| <b>Revised Plan Sheets</b> |                                                                                                                        |
|----------------------------|------------------------------------------------------------------------------------------------------------------------|
| Plan Sheet                 | Plan Sheet Title (brief description of changes to sheet)                                                               |
| 4                          | General Notes – Revised multiple utility contacts                                                                      |
| 43                         | Construction Details: Urban Underdrain – Removing note that open graded material is incidental to pipe underdrain item |
| 60                         | Pond Emergency Spillway Riprap revised to Riprap Medium                                                                |
| 62                         | Pond Emergency Spillway Riprap revised to Riprap Medium                                                                |
| 223                        | Storm Sewer: Madison Street – Elevation correction to Structure No. 750                                                |
| 457                        | Traffic Control: Stage 2A Madison Street – Added note                                                                  |
| 481                        | Traffic Control: Stage 2E Madison Street – Added note                                                                  |
| 554                        | Miscellaneous Quantities – Revised Removing Curb & Gutter quantity                                                     |
| 557                        | Miscellaneous Quantities – Excavation Rock added to Earthwork Summary                                                  |
| 558                        | Miscellaneous Quantities – Excavation Rock added to Earthwork Summary                                                  |
| 565                        | Miscellaneous Quantities – Revised Concrete Curb & Gutter 30-Inch Type A quantity                                      |

| <b>Revised Plan Sheets</b> |                                                                                                                                                       |
|----------------------------|-------------------------------------------------------------------------------------------------------------------------------------------------------|
| Plan Sheet                 | Plan Sheet Title (brief description of changes to sheet)                                                                                              |
| 570                        | Miscellaneous Quantities – Added schedule for Storm Sewer Rock Excavation and heading for bid items under Surface Drain Items                         |
| 599                        | Miscellaneous Quantities – SS item revision                                                                                                           |
| 610                        | Miscellaneous Quantities – SS item revision                                                                                                           |
| 642                        | Miscellaneous Quantities – Revise bid item quantities for Fence Chain Link Polymer Coated 4-FT; Added the schedule for Temporary Precast Trench Drain |
| 694,695,697                | Miscellaneous Quantities – Revised majority of traffic control quantities for revised staging durations                                               |
| 769                        | Plan: Sheet Legend – Revise legend for fence items                                                                                                    |
| 794                        | Plan: Waukesha Bypass – Revise fence note                                                                                                             |
| 800                        | Plan: Waukesha Bypass – Revise fence note                                                                                                             |
| 837                        | Plan: Waukesha Bypass – Revise fence note                                                                                                             |
| 840                        | Plan: Waukesha Bypass – Revise fence note and callout                                                                                                 |
| 903                        | Plan: Waukesha Bypass – Revise callout for fence type                                                                                                 |
| 1280                       | B-67-353 – Adding conduit details sheet to list of drawings                                                                                           |
| 1281                       | B-67-353 – Revised schedule of quantities for conduit                                                                                                 |
| 1290                       | B-67-353 – Showing conduit location in parapet wall                                                                                                   |
| 1320                       | B-67-355 – Adding conduit details sheet to list of drawings                                                                                           |
| 1321                       | B-67-355 – Revised schedule of quantities for conduit                                                                                                 |
| 1331                       | B-67-355 – Showing conduit location in parapet wall                                                                                                   |
| 1359                       | B-67-361 – Adding conduit details sheet to list of drawings                                                                                           |
| 1360                       | B-67-361 – Revised schedule of quantities for conduit                                                                                                 |
| 1370                       | B-67-361 – Showing conduit location in parapet wall                                                                                                   |
| 1406-1407                  | R-67-145 – Revised fencing adjacent to structure                                                                                                      |
| 1418-1420                  | Earthwork Data – Added Excavation Rock                                                                                                                |
| 1436                       | Earthwork Data – Added Excavation Rock                                                                                                                |
| 1440                       | Earthwork Data – Added earthwork data table for Division 2 - Driveway 222+76                                                                          |
| 1462-1583                  | Cross Sections: Waukesha Bypass – Added approximate ground water elevation added to cross sections                                                    |

| <b>Added Plan Sheets</b> |                                                                                                            |
|--------------------------|------------------------------------------------------------------------------------------------------------|
| Plan Sheet               | Plan Sheet Title (brief description of why sheet was added)                                                |
| 43A                      | Construction Details: Temporary Precast Trench Drain – Added to plans for Temporary Genesee Road Crossover |
| 1297A                    | B-67-353 – Conduit details                                                                                 |
| 1337A                    | B-67-355 – Conduit details                                                                                 |
| 1379A                    | B-67-361 – Conduit details                                                                                 |
| 1432A                    | Earthwork Data – Added earthwork data table for Division 7 - Macarthur Road                                |

The responsibility for notifying potential subcontractors and suppliers of these changes remains with the prime contractor.

Sincerely,

*Mike Coleman*

Proposal Development Specialist  
Proposal Management Section

**ADDENDUM NO. 01**

**2788-00-71**

**July 2, 2018**

**Special Provisions**

**5. Prosecution and Progress**

*Replace entire section titled Interim and Final Completion of Work with the following:*

**Interim and Final Completion of Work**

**Interim Completion of Stage 2B Work**

CTH D /Sunset Drive may be closed and detoured one time for a maximum of 74 calendar days in Stage 2B. If the contractor fails to complete the work necessary to reopen Sunset Drive during Stage 2B, to through traffic within 74 calendar days from the time closed, the department will assess the contractor \$4,000 in interim liquidated damages for each calendar day that the roadway remains closed beyond 74 calendar days. An entire calendar day will be charged for any period of time within a calendar day that the road remains closed beyond 12:01 AM.

**Interim Completion of Stage 2C Work (Station 111+00 to Station 112+63)**

Genesee Road may be closed and detoured one time for a maximum of 17 calendar days in Stage 2C to allow construction between Station 111+00 to Station 112+63. If the contractor fails to complete the work necessary to reopen Genesee Road during Stage 2C to through traffic within 17 days from the time closed, the department will assess the contractor \$12,000 in interim liquidated damages for each calendar day that the roadway remains closed beyond 17 calendar days. An entire calendar day will be charged for any period of time within a calendar day that the road remains closed beyond 12:01 AM.

**Interim Completion of Stage 2D Work**

Genesee Road Station 31+11 to Station 50+00 and Saylesville Road, Station 7+50 to Station 112+35, may be closed and detoured one time, for a maximum of 74 calendar days, between June 11, 2019 and August 23, 2019 in Stage 2D. If the contractor fails to complete the work necessary to reopen eastbound Genesee Road and Saylesville Road during Stage 2D to through traffic prior to 12:01AM August 24, 2019, the department will assess the contractor \$12,000 in interim liquidated damages for each calendar day that these roadways remain closed after 12:01AM August 24, 2019. An entire calendar day will be charged for any period of time within a calendar day that the road remains closed beyond 12:01 AM.

The department will not grant time extensions to the interim completion date specified above for the following:

1. Severe weather as specified in standard spec 108.10.2.2.
2. Labor disputes that are not industry wide.
3. Delays in material deliveries.

**Interim Completion of Stage 2E Work**

Merrill Hills Road may be closed and detoured one time for a maximum of 44 calendar days in Stage 2E. If the contractor fails to complete the work necessary to reopen Merrill Hills Road during Stage 2E to through traffic within 44 calendar days, the department will assess the contractor \$15,000 in interim liquidated damages for each calendar day that the roadway remains closed beyond 44 calendar days. An entire calendar day will be charged for any period of time within a calendar day that the road remains closed beyond 12:01 AM.

### **Final Completion of Work**

If contract time expires prior to completing all work specified in the contract, additional liquidated damages will be affixed according to standard spec 108.11.

*Replace entire section titled **Anticipated Schedule** with the following:*

### **Anticipated Schedule**

Do not move to the next stage until all work in the current stage is completed or as approved by the engineer except that Stage 2A work can overlap Stage 1 and other Stage 2 work except that the southern pavement areas needed for traffic control switches. Stage 2D and Stage 2E can overlap by 33 days, July 22, 2019 to August 23, 2019. Traffic control switches shall be completed during off peak hours.

*Replace paragraph four under section titled **Winter 2018** with the following:*

Any roadway construction which started during the 2018 construction season shall be completed through placing the base aggregate dense prior to winter.

*Replace the 6<sup>th</sup> bullet point under section titled **Stage 2E Construction (2019)** with the following:*

- Construct Waukesha Bypass northbound Station 254+75 to Station 255+62 LT and the east half of Madison Street Station 50+23 to Station 54+00.

## **6. Traffic**

*Replace entire section titled **Stage 2A Traffic** with the following:*

### **Stage 2A Traffic**

- CTH TT/Merrill Hills Road traffic maintained on existing alignment.
- CTH X, Saylesville Road has 2-way traffic on existing northbound lanes.
- STH 59/Genesee Road has 2-way traffic on existing eastbound lanes and widening.
- STH 59/Les Paul Parkway, has 2-way traffic on existing eastbound lanes.
- West side of Madison Street closed for reconstruction. Close only for the duration needed to complete the work in a timely and diligent manner, performing all construction operations in a continuous methodical manner to minimize the impacts to traffic.
- All other side roads to remain open.

*Replace entire section titled **Stage 2B Traffic** with the following:*

### **Stage 2B Traffic**

- CTH TT/Merrill Hills Road traffic remains on existing roadway north of STH 59.
- CTH D detoured.
- CTH X/Saylesville Road has 2-way traffic on existing northbound lanes.
- STH 59/Les Paul Parkway has 2-way traffic on existing eastbound lanes.
- Genesee Road has 2-way traffic on existing eastbound lanes and widening.

Replace entire section titled **Stage 2E Traffic** with the following:

#### **Stage 2E Traffic**

- CTH TT/Merrill Hills Road 2-way traffic maintained on existing lanes except closed and detoured between CTH D and Summit Avenue.
- CTH D traffic maintained on newly constructed lanes.
- STH 59 has 2-way traffic on northbound existing lanes.
- CTH X/Saylesville has 2-way traffic on eastbound existing lanes.
- Kame Terrace has 2-way traffic on westbound lanes in Stage 2E-1. Once that work is completed, Kame Terrace 2-way traffic is shift to the newly constructed eastbound lanes in Stage 2E-2.
- Madison Street has 2-way traffic on the west side of Madison Street while the east side of Madison Street is closed for reconstruction. Close only for the duration needed to complete the work in a timely and diligent manner, performing all construction operations in a continuous methodical manner to minimize the impacts to traffic.

#### **8. Utilities**

Replace entire article language with the following:

This contract comes under the provision of Administrative Rule Trans 220.  
107-065 (20080501)

Underground and overhead utility facilities are located within the project limits. Utility adjustments are required for this construction project as noted below. Coordinate construction activities with a call to Diggers Hotline or a direct call to the utilities that have facilities in the area as required per statutes. Use caution to ensure the integrity of underground facilities and maintain code clearances from overhead facilities at all times.

Contact each utility company listed in the plans, prior to preparing bids, to obtain current information on the status of existing and any new utility relocation work.

Utility companies will be performing utility work and adjustments within the limits and during the life of this project. The contractor shall cooperate and coordinate construction activities with these organizations.

If a conflict with discontinued utility facilities is encountered, contact the appropriate utility owner/representative for instructions on proper removal and disposal of said facility.

Some utility work described below is dependent on prior work being performed by the contractor at a specific site. In such situations, provide the engineer and the affected utility a good faith notice of when the utility is to start work at the site. Provide this notice 14 to 16 calendar days in advance of when the prior work will be completed and the site will be available to the utility. Follow-up with a confirmation notice to the engineer and the utility not less than 3 working days before the site will be ready for the utility to begin its work or as otherwise noted in this special provision.

Unless otherwise specified by the contract or authorized by the engineer, the existing utilities are to remain in service.

Contact the appropriate utility owner/representative prior to disturbing any discontinued facilities. Verify that utilities have been properly discontinued in place and do not necessitate any special requirements by the utility. The contractor shall not assume that unmarked facilities have been discontinued. At no time is it acceptable to push, pull, cut or drill an unmarked facility without explicit consent from the utility.

**AT&T Corporation** has the following facilities in the project area:

Underground facilities are located along the east side of Saylesville Road (CTH X) and south side of STH 59, east of CTH X, at the proposed realignment. There will be no conflicts with the proposed roadway work and utilities are to remain in place.

The construction field contact for AT&T Corporation is:

AT&T Legacy/Long distance  
 Mr. William Koenig  
 128 W. Sunset Ave.  
 Appleton, WI 54911  
 (608) 628-0575  
[wekoening@att.net](mailto:wekoening@att.net)

**AT&T Wisconsin** has underground and joint aerial facilities in the project corridor that require relocation or adjustment. AT&T Wisconsin will start their relocations in early April of 2018. Some of the relocations, as noted below, are dependent upon the other underbuilds completing their respective relocation work. AT&T Wisconsin will be performing their relocations prior to and during construction.

AT&T Wisconsin has the following facilities in the project area:

**Proposed Waukesha Bypass / CTH TT/**

| Station(s)                       | Work Proposed                                       | Description of Work                                                                                                                                                                                                                                                       |
|----------------------------------|-----------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| 216+75 LT/RT                     | Buried cable crossing CTH TT                        | Installation of pedestal at 216+75 RT will connect to existing pedestal on west side of proposed Waukesha Bypass.                                                                                                                                                         |
| 216+75 LT/RT to 230+00 LT/RT     | Disconnect Aerial(s)                                | Any aerials will be disconnected from service                                                                                                                                                                                                                             |
| 237+50                           | Buried cable crossing CTH TT                        | New buried cable to service Merrill Hills Court and existing line to remain in place                                                                                                                                                                                      |
| 237+50 LT to 255+89 LT           | Transfer of line service from underground to aerial | Buried cable to be discontinued in place on west side of existing CTH TT from 237+50 to 250+40, crossing at Station 250+40 and extending to 255+90 along the east side of CTH TT. Service to be established on new poles installed by WE Energies on west side of roadway |
| 237+50 LT to 250+50 LT           | Remove aerial line                                  | Remove aerial line along east side of existing.                                                                                                                                                                                                                           |
| 250+50<br>250+50 RT to 254+85 RT | Buried cable crossing CTH TT                        | Underground line crossing perpendicular to roadway with line then extending northeast to Madison Street at Station 52+50                                                                                                                                                  |
| 255+86                           | Aerial crossing                                     | New aerial crossing CTH TT just north of Madison Street intersection                                                                                                                                                                                                      |
| 255+89 LT to 264+15 LT           | Transfer of line service from underground to aerial | Buried cable to be discontinued in place on east side of CTH TT and service to be established on new poles installed by WE Energies on west side of roadway                                                                                                               |
| 264+15 LT/RT                     | Aerial crossing                                     | New aerial crossing CTH TT just north of Kisdon Hill Drive intersection                                                                                                                                                                                                   |

**Madison Street**

|       |                                               |                                                                                                                                                    |
|-------|-----------------------------------------------|----------------------------------------------------------------------------------------------------------------------------------------------------|
| 49+00 | Aerial crossing Madison Street east of CTH TT | Continuation of service being transferred from buried underground on the east side of CTH TT to new poles installed by WE Energies on west side of |
|-------|-----------------------------------------------|----------------------------------------------------------------------------------------------------------------------------------------------------|

|                                             |                           |                                                                                                                                |
|---------------------------------------------|---------------------------|--------------------------------------------------------------------------------------------------------------------------------|
|                                             | intersection              | roadway. Existing line to be discontinued in place.                                                                            |
| 47+00 LT to 49+00 LT                        | Relocate underground line | New underground line to be offset of existing line. Existing line to be discontinued in place on north side of Madison Street. |
| Dairy Avenue intersection to 49+00 RT       | Relocate underground line | New underground line to be offset of existing line. Existing line to be discontinued in place on south side of Madison Street. |
| 52+50 RT to east beyond construction limits | Relocate underground line | New underground line to be offset of existing line. Existing line to be discontinued in place on south side of Madison Street. |

AT&T Wisconsin estimates that they will require 6 weeks of construction time for the work outlined above. Their work will commence upon Charter, Windstream, MWFN and City of Waukesha Cable completing their relocations.

**Genesee Road / STH 59/Les Paul Parkway**

|                                                                |                                            |                                                                                                                                                                                                                                                                                           |
|----------------------------------------------------------------|--------------------------------------------|-------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| 32+40 LT/RT                                                    | Buried cable crossing Genesee Road         | Underground line crossing perpendicular to roadway with line then extending east on south side of Genesee Road                                                                                                                                                                            |
| 32+40 RT to 41+75 RT                                           | Buried cable                               | Buried underground line extending east on south side of Genesee Road, tying into cable that came from east leg of Genesee Road at Station 41+75                                                                                                                                           |
| 34+80 LT to 47+00 LT                                           | Aerial line and buried cable               | Aerial line will commence at Station 34+80 RT, extending east to Station 39+00 RT; buried cable will tie in from pole and continue east to new pedestal at Station 47+00 LT. Existing underground line from 32+50 to 52+00 along the west side of Genesee Rd to be discontinued in place. |
| 41+75 RT to 14+00 RT Saylesville R/L to 106+00 LT Les Paul R/L | Buried cable and crossing Saylesville Road | Line to be placed easterly along south R/W of Genesee Road, then turn south along west R/W line of Saylesville Road; line will cross perpendicularly under proposed Saylesville Road at 14+00; line will be placed along east R/W line of Saylesville Road to sta 106+00 LT               |
| 106+00 RT to 107+25 RT                                         | Buried cable and crossing Les Paul Parkway | Underground line crossing roadway.                                                                                                                                                                                                                                                        |
| 107+25 RT to 110+00 RT                                         | Buried cable                               | Underground line placed along north side of Les Paul Parkway R/W.                                                                                                                                                                                                                         |
| 52+00 RT to 59+50 RT                                           | Aerial line                                | New buried line to exit ground and continue on poles, supplanting buried discontinued cable remaining in place on west side of road                                                                                                                                                       |
| 59+50 RT to 61+75 RT                                           | Relocate underground line                  | New underground line to be placed east of Genesee Road, existing buried cable on west side of road to be discontinued in place                                                                                                                                                            |
| 61+75 LT/RT                                                    | Buried cable crossing Genesee Road         | Underground line crossing perpendicular to roadway then extending south on east side of Genesee Road                                                                                                                                                                                      |
| 7+25 LT to 9+75 LT Saylesville R/L                             | Buried cable to be lowered in place        | Buried cable to be lowered in place from 7+25 LT to 9+75 LT                                                                                                                                                                                                                               |

AT&T Wisconsin estimates that they will require 5 weeks of construction time for the work outlined above at the STH 59 intersection. Work is anticipated to start late May 2018.



**Sunset Drive / CTH D**

|                      |                                    |                                                                                                                               |
|----------------------|------------------------------------|-------------------------------------------------------------------------------------------------------------------------------|
| 43+00 LT to 47+80 LT | Relocate underground line          | New underground line to be offset of existing. Existing line to be discontinued in place on north side of existing Sunset Dr. |
| 47+80 LT/RT          | Buried cable crossing Sunset Drive | Underground line crossing perpendicular to roadway at Station 47+80                                                           |
| 47+80 RT to 61+50 RT | Relocate underground line          | New underground line to be offset of existing. Existing line to be discontinued in place on south side of existing Sunset Dr. |
| 61+50 RT to 65+50 RT | Relocate aerial line               | Aerial line to tie into existing at Station 65+50 RT                                                                          |

**MacArthur Road / Merrill Hills Road Intersection**

|                      |                                                     |                                                                                                                                    |
|----------------------|-----------------------------------------------------|------------------------------------------------------------------------------------------------------------------------------------|
| 76+30 LT to 77+75 LT | Transfer of line service from underground to aerial | Aerial line to replace buried service existing along west side of Merrill Hills Rd R/W. Existing line to be discontinued in place. |
| 56+50 RT to 58+50 RT | Relocate underground line                           | New line to cross at Station 76+30 of Merrill Hills Road. Existing line to be discontinued in place.                               |
| 76+90 RT to 78+50 RT | Relocate underground line                           | Buried line to cross MacArthur Road at Station 57+10. Existing line to be discontinued in place.                                   |

AT&T Wisconsin estimates that they will require 4 weeks of construction time for the work outlined above at Sunset Drive and MacArthur Roads. Work is anticipated to start early July 2018. The construction field contact for AT&T Wisconsin is:

Mr. Christopher Duncan  
 2005 Pewaukee Rd.  
 Waukesha, WI 53188  
 Work: (262) 896-7678  
 Cell: (414) 491-4810  
[cd8946@att.com](mailto:cd8946@att.com)

**City of Waukesha Sanitary and Storm Sewer** has underground facilities within the project limits and work is included as part of the proposed project improvements.

Contact Jonathan Schapekahn with the City of Waukesha at (262) 524-3584, [jschapekahn@waukesha-wi.us](mailto:jschapekahn@waukesha-wi.us) five business days prior to starting any work and to arrange for an inspector to be on site at the time work is completed.

Televise the sanitary sewer and place internal manhole sealing system as called for in the plans and special provisions.

| <b>Location and Conflict</b>                                                                                                                             | <b>Resolution</b>                                                                     |
|----------------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------|
| Station 254+45, 26' RT<br>Station 254+81, 4' LT                                                                                                          | Contractor to discontinue sanitary force main manholes as shown on the plans          |
| Station 268+12, 66' LT<br>Station 269+34, 66' LT<br>Station 269+65, 36' RT<br>Station 270+25, 36' RT                                                     | Contractor to adjust/reconstruct sanitary sewer manholes as shown on the plans        |
| Station 268+12, 66' LT to Station 269+34, 66' LT<br>Station 268+34, 66' LT to Station 269+65, 36' RT<br>Station 269+65, 66' LT to Station 270+25, 36' RT | Contractor will televise sanitary lines between the three listed stations and offsets |

|                        |                                                |
|------------------------|------------------------------------------------|
| Station 270+25, 36' RT | Contractor will televise sanitary lateral line |
|------------------------|------------------------------------------------|

The construction field contact for City of Waukesha Public Works (Sanitary) is:

Mr. Jonathan Schapekahm  
 130 Delafield Street  
 Waukesha, WI 53188  
 (262) 524-3584  
[jschapekahm@waukesha-wi.us](mailto:jschapekahm@waukesha-wi.us)

**City of Waukesha Public Works (Fiber Optics)** has overhead and underground facilities within the project limits. Generally the cable is overhead located on We Energies poles.

The City of Waukesha has aerial cable on WE Energies poles on the east side of Genesee Road from Station 34+11 RT to Station 42+35 RT. The aerial facility is currently attached to the WE Energies pole line that continues southeast crossing Saylesville Road at Station 16+40. This existing aerial cable crossing Saylesville Road at Station 16+40 will now cross at approximately Station 13+40, connecting to new WE Energies pole at Station 12+90, 80 feet RT. Aerial service will then reconnect to pole at Station 15+90, 370' RT. City of Waukesha estimates that they will require 2 weeks of construction time for the work outlined above. Their work will commence upon TCA and Charter completing their relocations.

The City of Waukesha has aerial cable on WE Energies poles on the east side of Saylesville Road from Station 7+50 RT to Station 15+88, 339 feet RT. Starting at the WE Energies pole at Station 15+88, 339 feet RT the fiber optic cable is in underground duct to the north and crosses Les Paul Parkway at approximately Station 106+88. The underground fiber optic enters a vault on the east side of Les Paul Parkway at Station 107+07, 80 feet RT. These will remain in place without adjustment.

The underground fiber optic line continues northwest along the east side of Les Paul Parkway until the intersection with Genesee Road where the duct continues north along the east side of Genesee Road. The end of this underground fiber optics line is a WE Energies pole near Pebble Creek at approximately Station 55+21, 101 feet RT. These will remain in place without adjustment.

The City of Waukesha has aerial cable on WE Energies poles from this pole at Station 55+21 on the east side of Genesee Road continuing through the project limits at Station 61+84. These will remain in place without adjustment.

Except for the crossing of Les Paul Parkway the majority of this underground fiber optic duct is located beyond the exterior limits of the construction work. The crossing of Les Paul Parkway will be ten feet higher after roadway construction is finished. This underground facility will remain in place without adjustment.

The City of Waukesha will be placing aerial cable on new We Energies poles from Station 253+99, 98 feet LT to Station 264+15, 79 feet LT. There will be 2 aerial crossings of the Bypass at Stations 264+15 and 253+99. There will be 2 aerial crossings of Madison Street at Stations 49+05 and 51+60. Along the north side of Madison Street, aerial line will be relocated to the new We Energies poles. City of Waukesha estimates that they will require 3 weeks of construction time for the work outlined above. Their work will commence upon Charter, Windstream and MWFN completing their relocations

The construction field contact for the City of Waukesha is:

Mr. Jeffrey Hernke  
 130 Delafield St.

Waukesha, WI 53188-3616  
(262) 524-3592  
jhernke@ci.waukesha.wi.us

**City of Waukesha Public Works (Street Lighting)**

The City of Waukesha will be responsible for inspection and approval of all City of Waukesha street lighting work. New lighting will be completed as a part of this project.

The City of Waukesha leases two street lights from We Energies within the project limits. The lights are mounted on existing We Energies wood poles. The locations are; Kisdon Hill Dr. & CTH TT and Madison St. & CTH TT. These lights are to remain as is and operational until they must be removed for construction progress. Please contact, Jeffrey Hernke, City of Waukesha DPW, 262-524-3592, thirty days in advance to schedule the removal. If notice is given less than thirty days, delays may occur with the removal.

Lighting work will begin with single luminaires installed in the southeast and northeast quadrant and twin luminaries installed at the median openings of the Waukesha Bypass and Kisdon Hill Drive. Conduit will extend in the median and connecting to the two twin luminaries installed under Project 2788-00-70/72 at the median opening of the Fiddlers Creek Drive intersection.

Contact Jeff Hernke with the City of Waukesha at (262) 524-3592, [jhernke@ci.waukesha.wi.us](mailto:jhernke@ci.waukesha.wi.us) five business days prior to performing the work to arrange for an inspector to be present at the time the work is completed.

**Level 3 Communications (Long Haul Fiber Optic Cable)** has an underground facility which runs parallel with the current Glacial Drumlin Trail. Due to this fiber optic cable being part of the L3 long haul network, it is imperative that this fiber does not take a hard cut/outage, nor add any additional and unnecessary splice enclosures. Level 3 will be performing their relocations during construction, prior to the construction of the Glacial Drumlin Trail box culvert.

| Location and Conflict                                                                                                    | Resolution                                                                                                                                                                                                                                |
|--------------------------------------------------------------------------------------------------------------------------|-------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| Station 48+35 10' LT to approximately Station 51+15 14' LT along and beneath the north side of the Glacial Drumlin Trail | Replace the fiber cable and conduit from an existing L3 hand hole to an existing L3 hand hole. Once the proposed L3 fiber cable is spliced in, the existing L3 fiber cable will be removed and the existing conduit will be discontinued. |

The construction field contacts for Level 3 Communications are:

Mr. Brahim Gaddour  
3235 Intertech Dr., Suite 600  
Brookfield, WI 53045  
Work: (414) 908-1027  
Cell: (414) 704-1026  
[brahim.gaddour@level3.com](mailto:brahim.gaddour@level3.com)

Network Relocations  
Mr. Masood Zeerak  
1025 Eldorado Blvd.  
Broomfield, CO 80021  
[level3.networkrelocations@level3.com](mailto:level3.networkrelocations@level3.com)

**Midwest Fiber Network (communication)** has aerial facilities attached to We Energies poles from Station 253+73, 110' RT to Station 264+27, 95' RT. Midwest Fiber Network will build new strand on the new We Energies poles along the west side of the Bypass and lash new fiber to the strand. The line will cross the Bypass at Stations 264+15 and 253+99. It will cross Madison Street at Stations 49+00 and 51+55, connecting to newly constructed We Energies poles along the north side of Madison Street east to Station 54+25 LT. The aerial line will be relocated to 14 new We Energies poles for this project. This work is to be done prior to and during roadway construction.

MWFN estimates that they will require 4 weeks of construction time for the work outlined above. Their work will commence upon Charter, and Windstream completing their relocations.

The construction field contact for Midwest Fiber Network is:

Mr. Cory Schmuki  
OSP Engineer  
6070 N. Flint Road  
Glendale, WI 53209-3714  
(414) 459-3554  
[cschmuki@midwestfibernetworks.com](mailto:cschmuki@midwestfibernetworks.com)

**Sprint Fiber (communication)** has an existing Fiber currently along the south side of the Glacial Drumlin State Trail.

| Location and Conflict                                                                                          | Resolution                                                                                                                                                                                                                                                                                                                      |
|----------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| South side of the Glacial Drumlin State Trail. This facility will be lowered to avoid EBS and marsh excavation | Relocation of this fiber line by directional boring approximately 2,475 linear feet at a lower elevation to clear construction. The fiber line crosses the Waukesha bypass reference line at Station 175+25. The fiber line will be placed 20 feet below the proposed roadway. Work will be done prior to roadway construction. |

Sprint will be performing their relocations during construction, prior to the construction of the Glacial Drumlin Trail box culvert.

The construction field contact for Sprint Fiber is:

Dan Hilliard  
Facilities/OSP Engineering  
849 Earl Street  
Saint Paul, MN 55106  
O: 651-772-6714  
M: 612-217-3526  
[dan.j.hilliard@sprint.com](mailto:dan.j.hilliard@sprint.com)

Kenneth M. Nine  
Project Manager  
JMC Engineers & Associates, Inc.  
110 N. Main Street  
Culver, IN 46511  
Office: 574-842-8830

Cell: 574-904-6336  
[kennethnine@att.net](mailto:kennethnine@att.net)

**Teleport Communication America –TCA (communication)** has aerial facilities that will be relocated in conjunction with We Energies pole relocations. Work is to begin as soon as WE Energies completes work request 3955541. It is anticipated that TCA will require 14 working days to complete their construction. Locations of the relocations are listed below:

| Location and Conflict                                                                                       | Resolution                                                                                                                                                                                                                                                                                                                                                                                                                                                   |
|-------------------------------------------------------------------------------------------------------------|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| Aerial lines in conflict with proposed Waukesha Bypass, will need to be relocated to new We Energies poles. | <p><b><u>STH 59 / Genesee Rd</u></b><br/>Station 32+34, 60 feet LT to Station 41+15, 185 feet RT, crossing Genesee Rd at Station 34+70.</p> <p><b><u>CTH X / Saylesville Rd</u></b><br/>Station 12+90, 80 feet RT<br/>Station 13+91, 78 feet LT<br/>Station 17+33, 132 feet LT<br/>Station 15+90, 370 feet RT</p> <p><b><u>STH 59 / Les Paul Pkwy</u></b><br/>Station 106+04, 142 feet LT<br/>Station 106+00, 132 feet RT to Station 109+05, 138 feet RT</p> |

The construction field contact for Teleport Communication America is:

Ms. Jennifer Navarro  
Northwind Technical Services  
6070 N Flint Rd  
Glendale WI, 53209  
Work: (414) 459-3564  
Cell: (414) 651-0036  
[j.navarro@northwindtech.com](mailto:j.navarro@northwindtech.com)

**Charter (communication)** has underground and overhead facilities within the project limits. Some of Spectrum/TWC/Charter's facilities are located on We Energies poles throughout the project limits. Relocations and adjustments of Spectrum facilities are anticipated to begin prior to the start of this roadway project.

Contact Scott Dietrich by phone at (414) 277-4283 a minimum of ten working days prior to working in the vicinity of Spectrum facilities.

At no time should the facility be used as a brace to support equipment or sheeting/shoring materials.

The following facilities have been identified for relocation:

**Waukesha Bypass (CTH TT)**

- Station 213+97, 126' RT to Station 213+52, 115 feet LT, new aerial crossing onto new We Energies poles.
- Station 213+52, 115' LT to Station 228+27, 151 feet LT remove existing aerial line on east side of Merrill Hills Road and install new aerial line on new We Energies poles on west side of the Bypass.
- Station 220+68, LT underground cable from Kame Terrace connect to new We Energies pole at Station 220+46, 120 feet LT.

- Station 224+00, install underground cable from new We Energies pole at Station 223+68, 150 feet LT to new pedestal at 224+46, 188 feet LT.
- Station 228+27, 151 feet LT to 237+00, 100 feet LT replace aerial on existing We Energies pole.
- Station 237+00, 100 feet LT install new underground line from new We Energies pole to new pedestal at 236+58, 106 feet LT.
- Station 237+00, 100 feet LT to 264+14, 100 feet LT – remove existing aerial line on east side of Merrill Hills Road and install new aerial line on new We Energies poles on west side of the Bypass.
- Station 243+11, 129 feet LT to Station 243+45, 100 feet RT new aerial line crossing on We Energies poles.
- Station 243+45, 100 feet RT to 245+00, 102 feet RT, install joint underground line cable with We Energies. At 245+00, 150 feet RT, expose and relay existing underground line to new pedestal at 102 feet RT.
- Station 248+76 RT to 252+21, 152 feet RT, replace underground line cable.
- Station 253+99, 98 feet LT to Station 254+01, 151 feet RT new aerial line crossing on We Energies poles.
- Station 255+60 to 257+07, 92 feet RT new underground line 15 feet from right-of-way.
- Station 264+14, 100 feet LT to Station 264+27, 90 feet RT new aerial line crossing on We Energies poles.

#### **Madison Street**

- Station 51+60 aerial crossing of Madison St on new We Energies poles
- Station 51+60 to 54+85 RT, relocate aerial to new We Energies poles.
- Station 47+45 to Station 49+00 RT, joint underground with We Energies, crossing Madison St at Station 47+45.

Charter estimates that they will require 6 weeks of construction time for the work outlined above. Their work will commence upon We Energies completing their relocations for work requests 3948247, 3955536, 3955542 and 3955543.

#### **Waukesha Bypass**

- Relocate aerial line crossing from Station 106+00 to Station 106+80 onto new We Energies poles.
- Station 133+00 to Station 135+00, aerial line to be removed.
- Station 151+70, LT, aerial line and pedestal to be removed.

#### **Genesee Road / STH 59**

- Station 34+65, no conflict with existing underground crossing. Approximately 4 foot vertical clearance.
- Station 34+71, 52.5 feet LT, adjust exiting pedestal to proposed grade and install new power supply at/near Station 34+70, 60 feet LT.
- Power supply pedestal at Station 35+35, 49 feet LT to be removed.

#### **Saylesville Road**

- Relocate aerial line crossing from Station 17+33, 132 feet LT to 12+90, 80 feet RT.

Charter estimates that they will require 6 weeks of construction time for the work outlined above. Their work will commence upon We Energies completing their relocations for work requests 3955540 and 3955541.

#### **Waukesha Bypass (CTH TT)**

- Station 176+54 LT/RT, replace aerial crossing onto new We Energies poles.

#### **Merrill Hills Road at WI & Southern Railroad Crossing**

- Existing aerial line crossing. Transfer existing risers to new poles north and south side of railroad crossing.

#### **Merrill Hills Road at MacArthur Road**

- Station 56+38, 53 feet LT to Station 57+28, 24 feet RT relocate aerial line to new We Energies poles.

Charter estimates that they will require 6 weeks of construction time for the work outlined above. Their work will commence upon We Energies completing their relocations for work request 3955538.

The construction field contact for Charter is:

Scott Dietrich  
Utility Coordinator  
1320 N. Dr. Martin Luther King Jr. Drive  
Milwaukee, WI 53212  
(414) 277-4283  
[Scott.Dietrich@Charter.com](mailto:Scott.Dietrich@Charter.com)  
[wis.engineering@charter.com](mailto:wis.engineering@charter.com)

**Waukesha Water Utility (Water)** - As part of this construction project, various components at various locations of water main work will need to be completed.

The work will consist of the following items: 16-Inch water main relocation, 12-Inch water main relocation, 8-Inch water main relocation, five hydrant relocations, four 20-Inch valve adjustments, four 16-Inch valve adjustments, one 12-Inch valve adjustment, two 8-Inch valve adjustments, five 6-Inch valve adjustments and installation of water main insulation.

Contractor shall coordinate work with the Waukesha Water Utility.

Existing valves and hydrants will be operated only by Waukesha Water Utility personnel or in the presence of the inspector, as authorized by Waukesha Water Utility.

All work associated with connecting the offset water main relocations to the old water main shall be coordinated with the Waukesha Water Utility as set forth in the plans and special provisions. The Waukesha Water Utility will assist in turning the existing valves to isolate these areas for the installation of the water main offsets.

Water mains will not be allowed to be shut down before 8:00 AM. Contractor is responsible to notify all customers when their water will be shut off and in case of an emergency. The notification should be done at least 24 hours prior to shut down whenever possible. No extra costs or change orders will be allowed for down time associated with the Waukesha Water Utility crews turning the water off or on.

Following construction but before the surface course, the Water Utility must be contacted to inspect their facilities on the project. Any damage found to be caused by the contractor shall be repaired by the contractor in a timely manner at the contractor's expense. All Water Utility facilities and other street structures must be exposed before the pavement is placed.

Contractor shall identify proposed grade changes that affect valve boxes or curb stops and notify Waukesha Water Utility providing at least 24 hour notice. Provide 72-hour notice if adjustment by the Waukesha Water Utility is necessary.

A schedule showing tentative dates for water main construction shall be provided to the water utility at least 2 weeks prior to beginning construction. Contractor shall provide 72 hours' notice of the anticipated need for inspection services. Contact Chris Walter with the Waukesha Water Utility at

(262) 409-4432, [cwalter@waukeshacounty.gov](mailto:cwalter@waukeshacounty.gov) to arrange for an inspector to be present at the time the work is completed. No work shall be undertaken without a Waukesha Water Utility inspector being on site without the permission of the owner. Payments may be denied, or removal of work may be ordered, for work accomplished without an inspector present or without the approval of the owner. Verify water main size, material, and elevation prior to proceeding with relocation work.

The construction field contact for Waukesha Water utility is:

Mr. Chris Walter  
115 Delafield St.  
Waukesha, WI 53188  
(262) 409-4432  
[cwalter@waukeshacounty.gov](mailto:cwalter@waukeshacounty.gov)

**We Energies – Electric** – We Energies has both aerial and underground electric facilities throughout the entire length of the project. These facilities will be relocated prior to construction.

Relocations and adjustments of We Energies facilities will be constructed per the following work requests: 3948247, 3955536, 3955538, 3955540, 3955541, 3955542, and 3955543. Highway stationing has been used where possible to locate new facilities.

New We Energies facilities installed in easement on private property may not be included in the following list, unless otherwise stated.

It is expected that contractors will work safely around any facilities left within the work zone. If plans change such that facilities become in conflict, it is expected that you will work with We Energies to resolve said conflict.

We Energies work is expected to begin in April 2018. Work is anticipated to take a total of 100 working days. All work is to be done prior to and during roadway construction. We Energies has provided a schedule of anticipated dates of completion of their work. The dates assume no lost work days due to inclement weather, wet spring work or emergency power restoration due to storms. Pole removals are dependent upon the removal of communication companies' facilities (underbuilds) from We Energies poles. Upon relocations by the underbuilds, We Energies will remove the poles.

We Energies – Electric plans to perform the following work, supplanted with the respective tables in the work plans:

**Contractor must call We Energies 24-hour Dispatch lines to arrange for verification.  
We Energies Electric Dispatch, (800) 662-4797  
We Energies Gas Dispatch, (800) 261-5325**

### **Overhead (#3948247)**

#### **CTH TT**

- Relocate aerial power line across CTH TT at Station 264+27, 90 feet RT to Station 264+11, 210 feet LT.
- Relocate existing aerial power lines from Station 238+82, 121 feet LT to Station 264+31, 45 feet RT on east side of existing roadway to west side of proposed Waukesha Bypass.
- Place aerial power line across CTH TT at Station 260+96.
- Place aerial power line across CTH TT at Station 255+89.
- Place aerial power line across CTH TT at Station 253+99.
- Place aerial power line across Madison Street at Station 51+60.
- Place aerial power line across CTH TT at Station 246+72.
- Place aerial power line across CTH TT at Station 243+25.



- Service to be disconnected due to demolition for W270 S 1990 Merrill Hills Road and pole to be removed at Station 239+57, 36 feet RT.

#### **Overhead (#3955536)**

##### **CTH TT**

- Relocate existing aerial power lines from Station 83+76 (R/L of Merrill Hills Road), 25 feet RT to Station 228+27, 151 feet LT on east side of existing roadway to west side of proposed Waukesha Bypass.
- Place aerial power line across CTH TT at Station 237+00
- Relocate existing pole from Station 234+75, 138 feet LT to Station 235+25, 130 feet LT. Relocate existing aerial power from Station 237+00 to Station 238+82, 121 feet LT on east side of existing roadway to new poles on west side of proposed Waukesha Bypass.

#### **Overhead (#3955543)**

##### **Madison Street/CTH TT**

- Relocate aerial power lines to underground from Station 47+50, 26 feet LT to Station 52+50, 48 feet LT.
- Place aerial power lines from Station 51+60, 48, LT to Station 55+93, 28 feet RT.

#### **Underground (#3948247)**

##### **CTH TT**

- Trench cable from Station 252+60, 141 feet RT to existing pedestal at Station 252+27 146 feet RT. (#3948247)
- Bore electrical cable from Station 243+45, 100 feet RT to Station 245+20, 111 feet RT. New cable to be spliced and tied in existing underground cable. Cable from Station 245+01, 67 feet RT to splice point to be discontinued.

#### **Underground (#3955536)**

##### **CTH TT**

- Trench cable from Station 215+57, 117 feet LT to splice point of existing underground cable at Station 215+64, 118 feet LT.
- Trench cable from Station 216+85, 119 feet LT to splice point of existing underground cable at Station 216+90, 136 feet LT.
- Trench cable from Station 216+87, 146 feet RT to splice point of existing underground cable at Station 217+02, 147 feet RT.
- Underground cable crossing CTH TT at Station 216+85 to be discontinued.
- Underground cable from Station 216+85 to approximately Station 220+25 on the west side CTH TT then crossing under to the east at Station 220+70 to be discontinued.
- Trench cable from Station 223+68, 150 feet LT to splice point of existing underground cable at Station 224+46, 188feet LT.
- Bore electrical cable to connect between existing pedestal located at southwest property corner of 604 Mesa Ct and northwest property corner of 608 Mesa Court to new pole installed for service of building for property address W270 S1920.
- Trench cable from Station 237+00, 100 feet LT to splice point of existing underground cable at Station 236+62, 108 feet LT.

#### **Underground (#3955542)**

##### **CTH TT/Madison Street**

- Trench cable from Station 53+67, 58 feet LT to new pad mount transformer at Station 54+12, 53 feet LT.

- Bore cable from Station 54+12, 53 feet LT to Station 257+21, 96 feet RT. Cable will have four splice points changing direction from east-west to north-south.
- Trench cable from pole at Station 54+23, 37 feet LT to Station 53+67, 58 feet LT.
- Trench cable from pole at Station 52+58, 48 feet LT to Station 52+59, 55 feet LT.
- Bore cable from 52+59, 55 feet LT to Station 261+10, 96 feet RT. Cable will have three splice points changing direction from east-west to north-south.
- Trench cable from Station 261+10, 96 feet RT to Station 261+10, 86 feet RT.
- Trench cable from splice point servicing 108/110 Harrogate Drive at Station 257+08, 101 feet RT to Station 257+21, 96 feet RT.
- Trench cable from splice point servicing 112/114 Harrogate Drive at Station 257+55, 98 feet RT to Station 257+21, 96 feet RT.

### **Underground (#3955543)**

#### **CTH TT/Madison Street**

- Bore one 6-Inch diameter crossing Station 47+50 (R/L of Madison Street), Station 253+45, and Station 52+05 (R/L of Madison Street). Cable will be trenched from new pole installed at Station 52+20, 48 feet LT to Station 52+05, 40 feet LT. A locating pedestal will be installed at Station 253+45, 188 feet RT. Four splice points will be used in the cable to change direction underground.

We Energies construction on the work requests 3948247, 3955536, 3955542, and 3955543 is anticipated to occur between April and Mid-June of 2018. The dates assume no lost work days due to inclement weather, wet spring work or emergency power restoration due to storms. Pole removals are dependent upon the removal of communication companies' facilities (underbuilds) from We Energies poles. Upon relocations by the underbuilds, We Energies will remove the poles.

### **Overhead (#3955538)**

#### **Waukesha Bypass (Off Existing Alignment)**

- Install two new poles at Station 12+45 and Station 12+46 (R/L of Glacial Drumlin Trail, Southwest Connection) perpendicular to the trail, replacing two existing poles that would be in conflict.

#### **CTH TT**

- Relocate existing aerial power line crossing CTH TT from Station 56+39, 1 feet LT to Station 56+78, 40 feet RT. New aerial power line will cross over new proposed extension of MacArthur Road from northwest corner to southeast corner, Station 56+38, 53 feet LT to Station 57+28, 24 feet RT.
- Install new pole at Station 177+85, 85 feet RT to replace pole at Station 177+54, 87 feet RT.
- Install new poles on existing overhead lines on north and south sides of the Wisconsin Southern Railroad.

### **Underground (#3955538)**

#### **Waukesha Bypass (Off Existing Alignment)**

- Bore three parallel 6-Inch cables with a 5 foot on center spacing. Placing from west side of proposed Waukesha Bypass, crossing underneath to the east and terminating on south side of MacArthur Road. The cables will have three splice points between connection points of existing underground lines.

Center cable will have splice point with existing underground cable at Station 188+15, 106 feet LT to next splice pint at Station 185+22, 106 feet LT. Cable will then cross underneath proposed Waukesha Bypass where excavation below subgrade (EBS) is to occur. The cable

will have 2 feet of cover upon completion of EBS. Next splice point will be on east side of the proposed Waukesha Bypass at Station 185+22, 103 feet RT followed by another splice point at Station 51+28 (R/L of MacArthur Road), 56 feet RT and completing final connection to existing underground line at Station 53+68, 56 feet RT.

Existing underground line between the new connection points to be discontinued.

- Bore two parallel 6-Inch cables with a 5-foot on center spacing from Station 19+00 (R/L of Glacial Drumlin Trail, Southeast Connection) crossing Wisconsin Southern Railroad line 6 feet below the rail bed, tying into existing underground lines at Station A 176+78, 110 feet RT. The cables will have two splice points between connection points of existing underground lines. Two locating pedestals will be located at the connection points for each underground line south of the railroad line.

Existing underground line between the new connection points to be discontinued.

We Energies construction on the work request 3955538 is anticipated to occur between late-June and late July of 2018. The dates assume no lost work days due to inclement weather, wet spring work or emergency power restoration due to storms. Pole removals are dependent upon the removal of communication companies' facilities (underbuilds) from We Energies poles. Upon relocations by the underbuilds, We Energies will remove the poles. Work at the railroad area anticipated to occur during the month of August of 2018.

#### **Overhead (#3955540)**

##### **Waukesha Bypass (Off Existing Alignment)**

- Disconnect aerial power lines after service to S31 W26897 Sunset Road has been removed.
- Disconnect aerial power lines after service to S32 W26620 Hawthorne Hollow Drive has been removed.

##### **Sunset Drive**

- Relocate existing aerial power line on south side of Sunset to new poles at Station 63+54, 40 feet RT and Station 61+58, 48 feet RT.

We Energies construction on the work request 3955540 is anticipated to occur between early July and mid-July of 2018. The dates assume no lost work days due to inclement weather, wet spring work or emergency power restoration due to storms. Pole removals are dependent upon the removal of communication companies' facilities (underbuilds) from We Energies poles. Upon relocations by the underbuilds, We Energies will remove the poles.

#### **Overhead (#3955541)**

##### **Genesee Road/ Les Paul Parkway**

- Install aerial power line from Station 34+77, 60 feet LT to Station 36+47, 78 feet LT.
- Place aerial power line across Genesee Road at Station 37+35.
- Relocate aerial power line from Station 109+05, 92 feet RT to Station 106+04, 193 feet LT.
- Place aerial power line across proposed Waukesha Bypass at Station 106+00.
- Move existing light pole at Station 104+00, 56 feet RT.
- Remove aerial power from Station 47+59, 96 feet LT to 52+37 165 feet RT.

##### **Saylesville Road**

- Relocate aerial power line from Station 17+31, 134 feet LT to Station 16+90, 370 feet RT.
- Place aerial power line across proposed Saylesville Road at Station 13+36.
- Place aerial power line across proposed Saylesville Road at Station 17+20.

## **Underground (#3955541)**

### **Waukesha Bypass**

- Bore cable from Station 107+52, 143 feet RT to Station 103+50, 112 feet RT, splicing into existing underground line. Cable will have one splice point in-between at Station 104+88, 144 feet RT.

### **Genesee Road**

- Bore one 2-Inch diameter crossing Station 47+50 from Station 17+60 (R/L of Saylesville Road), 100 feet RT of new pole to Station 47+50, 120 feet LT of new pedestal. Two splice points will be in the line, one at Station 45+84, 216 feet RT and Station 47+50, 125 feet RT.
- Trench cable from Station 47+50, 120 feet LT to new pole at Station 47+60, 120 feet LT.

We Energies construction on the work request 3955541 is anticipated to occur between mid-July and early August of 2018. The dates assume no lost work days due to inclement weather, wet spring work or emergency power restoration due to storms. Pole removals are dependent upon the removal of communication companies' facilities (underbuilds) from We Energies poles. Upon relocations by the underbuilds, We Energies will remove the poles.

### **Work Request # 3948247 (Waukesha Bypass)**

| <b>Station No.</b>           | <b>Sequence No.</b> | <b>Pole No.</b> | <b>Work Proposed</b>                                             |
|------------------------------|---------------------|-----------------|------------------------------------------------------------------|
|                              | 100                 | -               | Wewauk Substation                                                |
| 264+14 L100                  | 110                 | 17-             | Install Pole                                                     |
| 263+72 L100                  | 112                 | 14-04383        | Pole to Remain                                                   |
| 264+11 L210                  | 114                 | 17-             | Install Pole and Anchor                                          |
| 264+15 L79                   | 120                 | 17-             | Install Pole - 3 feet Fill – grade around                        |
|                              | -                   | -               | Install Anchor – grade around                                    |
| 264+27 R90                   | 122                 | 17-             | Pole (Installed on WISDOT: 2788-00-72). Remove Temporary Anchors |
| 262+23 R86                   | 124                 | 17-             | Install Pole – 1-foot fill, grade around                         |
| 264+29 L47                   | 125                 | 05-17264        | Remove Pole                                                      |
| 264+31 R45                   | 127                 | 03-05591        | Remove Pole and Anchor                                           |
| 263+25 R31                   | 129                 | 85-03503        | Remove Pole and Anchor                                           |
| 263+84 L80                   | 130                 | 17-             | Install Pole – 2.5 feet Fill – grade around                      |
| 263+60 L100 to<br>263+55 L81 | 135 -140            | -               | Discontinue Cable                                                |
| 262+58 R22                   | 135                 | 62-3403         | Remove Pole                                                      |
| 262+53 L44                   | 139                 | 05-17263        | Remove Pole and Anchor                                           |
| 263+55 L81                   | 140                 | 17-             | Install Pole                                                     |
| 260+96 L81                   | 150                 | 17-             | Install Pole - 5 feet Fill – grade around                        |
| 261+10 R86                   | 152                 | 17-             | Install Pole                                                     |
|                              | -                   | -               | Install Anchors                                                  |
| 260+80 R30                   | 155                 | 57-1799         | Remove Pole                                                      |
| 258+49 L84                   | 160                 | 17-             | Install Pole                                                     |
|                              | -                   | -               | Install Anchor                                                   |
| 258+93 R41                   | 163                 | 62-3862         | Remove Pole                                                      |

| Station No.      | Sequence No. | Pole No. | Work Proposed                                          |
|------------------|--------------|----------|--------------------------------------------------------|
| 257+13 R70       | 165          | 63-3859  | Remove Pole                                            |
| 257+13 R81       | 167          | 86U4105  | Remove Ped                                             |
| 255+89 L91.5     | 170          | 17-      | Install Pole – 3.5 feet Cut – grade around             |
| 255+87 R91       | 172          | 17-      | Install Pole – 1 feet Cut – grade around               |
|                  | -            | -        | Install Anchor                                         |
| 253+99 L98       | 180          | 17-      | Install Pole and Anchor - 1.5 feet Fill – grade around |
| 254+01 R151      | 182          | 17-      | Install Pole                                           |
|                  | -            | -        | Install Anchor                                         |
| 254+04 R143      | 184          | -        | Splice                                                 |
| 253+92 R113      | 185          | 73-7293  | Remove Pole                                            |
| 51+60 L48        | 186          | 17-      | Pole Set WR# 3955543                                   |
| 252+31 L106      | 190          | 17-      | Install Pole – Set 3 feet deeper for Marsh Excavation  |
| 252+60 R141      | 192          | 17-      | Install Pole                                           |
|                  | -            | -        | Install Anchor                                         |
| 252+60 R141 to   | 192-194      | -        | Install Cable                                          |
| 252+27 R146      | 194          | 84U3041  | Pedestal to remain                                     |
| 252+22 R133      | 195          | 00-04724 | Remove Pole                                            |
| 250+44 L115      | 200          | 17-      | Install Pole                                           |
| 250+44 L100      | -            | -        | Install Anchor                                         |
| 250+42 R144      | 205          | 62-3895  | Remove Pole                                            |
| 248+56 L124      | 210          | 17-      | Install Pole                                           |
| 248+72 R118      | 215          | 62-3896  | Remove Pole                                            |
| 246+72 L129      | 220          | 17-      | Install Pole                                           |
|                  | -            | -        | Install Anchor                                         |
| 245+43 L129      | 230          | 17-      | Install Pole                                           |
| 245+01 R67       | 235          | 02-07917 | Remove Pole                                            |
|                  | 235-244      |          | Discontinue Cable                                      |
| 246+73 R90       | 222          | 13-02675 | Pole to Remain, Install Anchor                         |
| 243+11 L129      | 240          | 17-      | Install Pole                                           |
|                  | -            | -        | Install Anchor                                         |
| 243+45 R100      | 242          | 17-      | Install Pole                                           |
|                  | -            | -        | Install Anchor                                         |
| 243+45 to 245+20 | 242-244      | -        | Install Cable – 2 feet Extra Deep for Marsh Excavation |
| 245+20 R111      | 244          | -        | Splice Pit                                             |
| 243+49 R40       | 245          | 10-13376 | Remove Pole                                            |
| --               | 248          | 85U4872  | Transformer at 3028 Cone View Lane                     |
| 240+64 L127      | 250          | 17-      | Install Pole                                           |
| 241+63 L02       | 255          | 02-07232 | Remove Pole                                            |

| Station No. | Sequence No. | Pole No. | Work Proposed              |
|-------------|--------------|----------|----------------------------|
| 238+82 L121 | 260          | 17-      | Pole set on WR#3955536     |
| 239+55 L48  | 265          | 62-3908  | Remove Pole                |
| 239+57 R36  | 267          | 61-6610  | Remove Pole                |
| 238+45, L74 | 270          | 62-3909  | Pole Removed on WR#3955536 |

**Work Request # 3955536 (Merrill Hills Rd / Waukesha Bypass)**

| Station No.                                  | Sequence No. | Pole No. | Work Proposed                         |
|----------------------------------------------|--------------|----------|---------------------------------------|
| 82+70, 25 feet RT                            | 100          | 17-      | Install pole and anchors              |
| 215+53, 223 feet RT                          | 102          | 17-      | Install pole                          |
| 215+16, 217 feet RT                          | 103          | 62-3897  | Remove pole                           |
| 216+87 148 feet RT.                          | 104          | 17-      | Install pole, primary riser           |
| 216+88 127 feet RT                           | 105          | 62-3879  | Remove pole                           |
| 216+88 127 feet RT to<br>215+64, 118 feet LT | 105-152      | -        | Discontinue cable                     |
| 217+02, 152 feet RT                          | 106          | N/A      | Primary 1 phase splice                |
| 216+66, 94 feet RT                           | 109          | 88-11711 | Remove pole and anchor                |
| 213+97, 126 feet RT                          | 120          | 17-      | Install pole and anchor               |
| 213+52, 115 feet LT                          | 140          | 17-      | Install pole and anchors              |
| 215+57, 117 feet LT                          | 150          | 17-      | Install pole, 1 phase primary riser   |
| 215+64, 118 feet LT                          | 152          | N/A      | Primary splice, 1 phase               |
| 218+80, 72 feet RT                           | 155          | 78-7257  | Remove pole                           |
| 216+85, 119 feet LT                          | 160          | 17-      | Install pole, 1 phase primary riser   |
| 216+90, 136 feet LT                          | 162          | N/A      | Primary splice, 1 phase               |
| to                                           | 162-165      | -        | Discontinue cable                     |
| 220+70, 19 feet RT                           | 165          | 62-3877  | Remove pole                           |
| 218+63, 119 feet LT                          | 170          | 17-      | Install pole                          |
| 220+40, 120 LT                               | 190          | 17-      | Install pole and anchors, transformer |
| 221+28, 87 feet LT                           | 192          | 88-06892 | Pole to remain, grade around          |
| 222+65, 29 feet LT                           | 195          | 62-3876  | Remove pole                           |
| 222+40, 139 feet LT                          | 200          | 17-      | Install pole                          |
| 223+68, 150 feet LT                          | 206          | 17-      | Install pole                          |
| 224+46, 188 feet LT                          | 208          | N/A      | 1 phase, primary splice               |
| 225+32, 161 feet LT                          | 210          | 17-      | Install pole                          |
| 224+63, 76 feet LT                           | 215          | 62-3870  | Remove pole                           |
| 226+55, 168 feet LT                          | 220          | 17-      | Install pole and anchors              |
| 226+64, 118 feet LT                          | 223          | 75-2649  | Remove pole                           |
| 227+29, 134 feet RT                          | 225          | 99-10715 | Remove pole and anchor                |
| No Station                                   | 227          | 99-10716 | Remove pole                           |
| No Station                                   | 229          | 98-11117 | Remove pole                           |
| 228+24, 147'LT                               | 230          | 17-      | Install pole and anchors              |
| 228+27, 151' LT                              | 235          | 62-3857  | Remove pole                           |

| Station No.     | Sequence No. | Pole No. | Work Proposed                     |
|-----------------|--------------|----------|-----------------------------------|
| 234+75, 138' LT | 265          | 62-3885  | Remove pole                       |
| 235+25, 130' LT | 270          | 17-      | Install pole                      |
| 236+62, 108' LT | 285          | 62-3884  | Remove pole                       |
| 237+00, 100' LT | 290          | 17-      | Install pole                      |
| 237+00, 100'RT  | 292          | 17-      | Install pole                      |
| 237+00, 120'RT  | -            | -        | Install anchor (private property) |
| 237+00 - 236+62 | 290-294      | -        | Trench cable                      |
| 238+45, 74' LT  | 295          | 62-3909  | Remove pole                       |
| 238+82, 121' LT | 300          | 17-      | Install pole and anchor           |

**Work Request # 3955542 (Madison St, Waukesha Bypass)**

| Station No. | Sequence No. | Pole No. | Work Proposed                 |
|-------------|--------------|----------|-------------------------------|
| 54+23 L37   | 100          | 17-      | Pole Installed on WR# 3955543 |
|             | 100 - 102    | -        | Trench Cable                  |
| 54+12 L53   | 102          |          | Install Pad mount Transformer |
|             | 102 - 104    |          | Trench Cable                  |
|             | 102-106      | -        | Bore Cable                    |
| 53+67 L58   | 104          |          | Splice Pit                    |
| 52+59 L53   | 106          | -        | Splice Pit                    |
|             | 106 - 108    |          | Bore Cable                    |
| 52+59 L60   | 108          |          | Splice Pit                    |
|             | 108 - 110    |          | Bore Cable                    |
| 50+97 L60   | 110          |          | Splice Pit                    |
|             | 110-112      |          | Bore Cable                    |
| 256+00 L97  | 112          |          | Splice Pit                    |
|             | 112 - 120    | -        | Bore Cable                    |
| 257+22 R96  | 120          | -        | Install Pad mount Transformer |
|             | 120 - 122    | -        | Trench Cable                  |
| 257+08 R101 | 122          | -        | Splice Pit                    |
|             | 120 - 124    | -        | Trench Cable                  |
| 257+55 R98  | 124          | -        | Splice Pit                    |
| 52+58 L48   | 200          | -        | Pole Installed on WR# 3955543 |
|             | 200 - 202    | -        | Trench Cable                  |
| 52+59 L55   | 202          | -        | Splice Pit                    |
|             | 202 - 204    | -        | Bore Cable                    |
| 255+60 R99  | 204          | -        | Splice Pit                    |
|             | 204 - 206    | -        | Bore Cable                    |
| 256+00 R92  | 206          | -        | Splice Pit                    |
|             | 206-208      |          | Bore Cable 1 feet EXTRA DEEP  |
| 259+68 R86  | 208          |          | Splice Pit                    |
|             | 208 - 210    | -        | Bore Cable                    |

| Station No. | Sequence No. | Pole No. | Work Proposed                 |
|-------------|--------------|----------|-------------------------------|
| 261+10 R96  | 210          | -        | Splice Pit                    |
|             | 210 - 212    | -        | Trench Cable                  |
| 261+10 R86  | 212          | 17-      | Pole Installed on WR# 3948247 |

**Work Request # 3954443 (Madison St, Waukesha Bypass)**

| Station No.                 | Sequence No. | Pole No. | Work Proposed                          |
|-----------------------------|--------------|----------|----------------------------------------|
| 46+33 L26                   | -            | 99-10928 | Pole to Remain                         |
| 47+50 L26                   | 100          | 17-      | Install Pole                           |
| -                           | -            | -        | Install Anchor                         |
| 47+50 L26 to                | 100 to 102   | -        | Bore Cable                             |
| 47+50 R38                   | 102          | -        | Splice pit                             |
| 47+50 R38 to                | 102 to 104   | -        | Bore Cable                             |
| 254+82 L102                 | 104          | -        | Splice pit                             |
| 254+82 L102 to              | 104 to 106   | -        | Bore Cable                             |
| 48+12 L26                   | 105          | 88-06599 | Remove Pole                            |
| 253+45 L108                 | 106          | -        | Splice pit                             |
| 253+45 L108                 | 106 to 108   | -        | Bore Cable                             |
| 253+45 R188                 | 108          | -        | Splice pit – Install Locating Pedestal |
| 253+45 R188 to<br>52+05 L40 | 108 to 132   | -        | Bore Cable                             |
| 52+05 L40 to 52+20<br>L48   | 132-130      | -        | Trench Cable                           |
| 51+60 L48                   | 110          | 17-      | Install Pole                           |
| -                           | -            | -        | Install Anchor                         |
| -                           | -            | -        | Install Anchor                         |
| 46+59 L23                   | 115          | 99-05950 | Remove Pole                            |
| 46+59 L23 to<br>53+26 L29   | 115 to 145   | -        | Discontinue Cable                      |
| 51+90 L48                   | 120          | 17-      | Install Pole                           |
| 50+95 L35                   | 125          | 62-3860  | Remove Pole and Anchor                 |
| 52+20 L48                   | 130          | 17-      | Install Pole                           |
| 52+05 L40                   | 132          | -        | Splice                                 |
| 52+68 L31                   | 135          | 73-7291  | Remove Pole                            |
| 52+58 L48                   | 140          | 17-      | Install Pole                           |
| -                           | -            | -        | Install Anchor                         |
| 53+26 L29                   | 145          | 99-10926 | Remove Pole                            |
| 54+23 L37                   | 150          | 17-      | Install Pole                           |
| 54+22 L27                   | 155          | 73-7290  | Remove Pole                            |
| 55+96 L26                   | 160          | 73-7289  | Pole to Remain                         |
| 55+93 R28                   | 162          | 17-      | Install Pole                           |
| -                           | -            | -        | Install Anchor                         |



| Station No. | Sequence No. | Pole No. | Work Proposed  |
|-------------|--------------|----------|----------------|
| 56+31 R26   | 167          | 62-12169 | Remove Pole    |
| 57+76 L27   | 170          | 73-7288  | Pole to Remain |

**Work Request # 3955538 (Waukesha Bypass, MacArthur Rd, Merrill Hills Rd, Glacial Drumlin Trail)**

| Station No.                | Sequence No. | Pole No. | Work Proposed                                                        |
|----------------------------|--------------|----------|----------------------------------------------------------------------|
| 188+10 L101                | 100          | -        | Splice                                                               |
| 188+15 L106                | 102          | -        | Splice                                                               |
| 188+11 L111                | 104          | 17u      | Install Locating Pedestal                                            |
| 188+10 L101 to 185+27 L101 | 100 to 110   | -        | Bore Cable, 2 feet extra depth for Marsh Excavation 185+00 to 186+00 |
| 188+15 L106 to 185+22 L106 | 102 to 112   | -        | Bore Cable, 2 feet extra depth for Marsh Excavation 185+00 to 186+00 |
| 188+11 L111 to 185+17 L111 | 104 to 114   | -        | Bore Cable, 2 feet extra depth for Marsh Excavation 185+00 to 186+00 |
| 185+27 L101                | 110          | -        | Splice                                                               |
| 185+22 L106                | 112          | -        | Splice                                                               |
| 185+17 L111                | 114          | -        | Splice                                                               |
| 185+27 L101 to 185+27 R101 | 110 to 120   | -        | Bore Cable – extra depth for EBS Excavation                          |
| 185+22 L106 to 185+22 R103 | 112 to 118   | -        | Bore Cable – extra depth for EBS Excavation                          |
| 185+17 L111 to 185+17 R105 | 114 to 116   | -        | Bore Cable – extra depth for EBS Excavation                          |
| 185+17 R105                | 116          | -        | Splice                                                               |
| 185+22 R103                | 118          | -        | Splice                                                               |
| 185+27 R101                | 120          | -        | Splice                                                               |
| 185+17 R105 to 53+67 R64   | 116 to 128   | -        | Bore Cable                                                           |
| 185+22 R103 to 53+68 R56   | 118 to 130   | -        | Bore Cable                                                           |
| 185+27 R101 to 53+80 R51   | 120 to 132   | -        | Bore Cable                                                           |
| 51+30 R61                  | 122          | -        | Splice                                                               |
| 51+28 R56                  | 124          | -        | Splice                                                               |
| 51+26 R51                  | 126          | -        | Splice                                                               |
| 51+30 R61 to 53+67 R64     | 122 to 128   | -        | Bore Cable                                                           |
| 51+28 R56 to 53+68 R56     | 124 to 130   | -        | Bore Cable                                                           |
| 51+26 R51 to 53+80 R51     | 126 to 132   | -        | Bore Cable                                                           |
| 53+67 R64                  | 128          | -        | Splice                                                               |

| <b>Station No.</b>                  | <b>Sequence No.</b> | <b>Pole No.</b> | <b>Work Proposed</b>                                                |
|-------------------------------------|---------------------|-----------------|---------------------------------------------------------------------|
| 53+68 R56                           | 130                 | -               | Splice                                                              |
| 53+80 R51                           | 132                 | -               | Splice                                                              |
| 188+10 L101 to<br>53+80 R51         | 100 to 132          | -               | Discontinue Cable                                                   |
| 188+15 L106 to<br>53+68 R56         | 102 to 130          | -               | Discontinue Cable                                                   |
| 188+11 L111 to<br>53+67 R64         | 104 to 128          | -               | Discontinue Cable                                                   |
| 56+38, 53 feet LT                   | 140                 | 17-             | Install new pole and Anchor                                         |
| 56+39, 01 feet LT                   | 145                 | 98-07910        | Remove pole and light                                               |
| 57+28, 24 feet RT                   | 150                 | 17-             | Install new pole and Anchor                                         |
| 56+78, 40 feet RT                   | 155                 | 66-8797         | Remove pole                                                         |
| 182+26 L108                         | 164                 | -               | Customer Pole                                                       |
| 182+26 L108 to<br>70+83, 42 feet LT | 164 to 200          |                 | Remove Overhead Conductor                                           |
| 181+83, 58 feet LT                  | 175                 | 76-2258         | Remove pole                                                         |
| 180+33, 77 feet RT                  | 185                 | 76-2259         | Remove pole                                                         |
| 70+83, 42 feet LT                   | 200                 | 01-18738        | Pole to remain                                                      |
| 177+52, 85 feet RT                  | 210                 | 17-             | Install new pole W/9 feet of fill, grade<br>around pole             |
| 177+54, 87 feet RT                  | 215                 | 98-07910        | Remove pole                                                         |
| 176+57 R115                         | 212                 | -               | Splice                                                              |
| 176+57 R115 to<br>174+57 R118       | 212 to 216          | -               | Bore Cable – extra depth for Marsh<br>Excavation and Storm outfall. |
| 176+78 R110 to<br>174+55 R113       | 214 to 218          | -               | Bore Cable – extra depth for Marsh<br>Excavation and Storm outfall  |
| 174+57 R118                         | 216                 | -               | Splice                                                              |
| 174+55 R113                         | 218                 | -               | Splice                                                              |
| 174+57 R118 to<br>19+00 L63         | 216 to 222          | -               | Bore Cable                                                          |
| 174+55 R113 to<br>19+05 L68         | 218 to 220          | -               | Bore Cable                                                          |
| 19+05 L68                           | 220                 | -               | Splice                                                              |
| 19+00 L63                           | 222                 | -               | Splice                                                              |
| 19+05 L68 to<br>19+05 R97           | 220 to 310          | -               | Bore Cable – extra depth for Trail<br>Excavation                    |
| 19+00 L63 to<br>19+00 R90           | 222 to 300          | -               | Bore Cable – extra depth for Trail<br>Excavation                    |
| 45+85, 41 feet RT                   | 228                 | 71-5247         | Install anchor                                                      |
| 46+75, 39 feet RT                   | 230                 | 17-             | Install new pole                                                    |
| 10+85, 09 feet LT                   | 235                 | 71-5377         | Remove pole                                                         |
| 176+14, 34 feet LT                  | 237                 | 01-18737        | Remove pole                                                         |
| 12+45, 12 feet LT                   | 240                 | 17-             | Install new pole with 4 feet of fill,<br>grade around pole          |

| Station No.               | Sequence No. | Pole No. | Work Proposed                                                                                       |
|---------------------------|--------------|----------|-----------------------------------------------------------------------------------------------------|
| 12+44, 05 feet LT         | 245          | 01-18753 | Remove pole                                                                                         |
| 12+46, 15 feet RT         | 250          | 17-      | Install new pole with 2 feet of fill, grade around pole                                             |
| 12+44, 06 feet LT         | 255          | 01-18752 | Remove pole                                                                                         |
| 62+43, 64 feet LT         | 260          | 17-      | Install new pole                                                                                    |
| 62+44, 34 feet LT         | 270          | 17-      | Install new pole                                                                                    |
| 62+45, 31 feet LT         | 275          | 57-1641  | Remove pole                                                                                         |
| 174+23, 75 feet LT        | 277          | 10U7378  | Remove locating pedestal                                                                            |
| 174+17, 73 feet RT        | 280          | 17-      | Install new pole with 8 feet of fill, Set 3 feet extra deep for Marsh Excavation, grade around pole |
| 174+14, 66 feet RT        | 285          | 71-5106  | Remove pole                                                                                         |
| 172+92, 83 feet RT        | 290          | 17-      | Install new pole with 4 feet of fill, grade around pole                                             |
| 173+10, 65 feet RT        | 295          | 60-2326  | Remove pole                                                                                         |
| 19+00, 90 feet RT         | 300          | 17U      | Install electric pedestal                                                                           |
| 19+05, 97 feet RT         | 310          | 17U      | Install electric pedestal                                                                           |
| 19+75, 19 feet RT         | 320          | 17-      | Install new pole                                                                                    |
| 19+45, 02 feet RT         | 325          | 81-01167 | Remove pole                                                                                         |
| 176+57 R115 to 174+42 L85 | 212 to 277   |          | Discontinue 2 sets of Cables                                                                        |
| 174+42 L85 to 19+05 R97   | 277 to 310   |          | Discontinue 2 sets of Cables                                                                        |
| 19+05, 97 feet RT         | 310          | 17U      | Install electric pedestal                                                                           |
| 19+75, 19 feet RT         | 320          | 17-      | Install new pole                                                                                    |
| 19+45, 02 feet RT         | 325          | 81-01167 | Remove pole                                                                                         |
| 176+57 R115 to 174+42 L85 | 212 to 277   |          | Discontinue 2 sets of Cables                                                                        |
| 174+42 L85 to 19+05 R97   | 277 to 310   |          | Discontinue 2 sets of Cables                                                                        |

**Work Request # 3955540 (Sunset, Hawthorne Hollow, Waukesha Bypass)**

| Station No.       | Sequence No. | Pole No. | Work Proposed                                             |
|-------------------|--------------|----------|-----------------------------------------------------------|
| No Station        | 100          | 51-4159  | Pole to Remain, Add Anchor                                |
| No Station        | 115          | 51-4158  | Remove pole                                               |
| No Station        | 125          | 10-02265 | Remove pole                                               |
| No Station        | 135          | 96-17202 | Remove pole and guy/anchor                                |
| 61+58, 48 feet RT | 140          | 17-      | Install new pole and anchors                              |
| 61+58, 32 feet RT | 145          | 60-5584  | Remove pole and guys/anchors                              |
| 63+54, 40 feet RT | 150          | 17-      | Install new pole - Set 2 feet extra deeper – grade around |

| Station No.          | Sequence No. | Pole No. | Work Proposed                   |
|----------------------|--------------|----------|---------------------------------|
| 63+54, 34 feet RT    | 155          | 60-2172  | Remove pole                     |
| 65+52, 33 feet RT    | 160          | 60-5585  | Pole to Remain                  |
| No Station           | 170          | 06-08275 | Pole to Remain – Install Anchor |
| No Station           | 180          | 56-6241  | Pole to Remain, Top Pole        |
| 135+59, 103 feet' LT | 195          | 53-13694 | Remove pole                     |
| 134+91, 06 feet LT   | 205          | 56-6242  | Remove pole                     |
| 134+22, 86 feet RT   | 215          | 34-1040  | Remove pole and guys/anchors    |
| 132+96, 08 feet LT   | 217          | 56-6240  | Remove pole and anchor          |
| 132+01, 20 feet RT   | 219          | 46-3725  | Remove pole and guy/anchor      |
| 133+31, 204 feet' RT | 225          | 39-2588  | Remove pole and guy/anchor      |

**Work Request # 3955541 (Saylesville Rd, Genesee Rd, Waukesha Bypass)**

| Station No.                                | Sequence   | Pole No. | Work Proposed                                        |
|--------------------------------------------|------------|----------|------------------------------------------------------|
| 12+90, 80 feet RT                          | 100        | 17-      | Install Pole and Anchor                              |
| 12+88, 80 feet RT                          | 105        | 96-05347 | Remove pole                                          |
| 13+91, 78 feet RT                          | 110        | 17-      | Install Pole                                         |
| 17+33, 132 feet LT                         | 130        | 17-      | Install Pole and Anchors                             |
| 17+31, 134 feet LT                         | 135        | 97-10307 | Remove Pole and Anchors                              |
| 16+18, 72 feet RT                          | 137        | 97-10308 | Remove pole                                          |
| 16+90, 370 feet RT                         | 140        | 96-05346 | Pole to Remain – Remove Anchor                       |
| 34+77, 60 feet LT                          | 150        | 17-      | Install new pole – 2 feet Fill Grade Around          |
| 34+79, 54 feet LT                          | 155        | 98-12869 | Remove pole                                          |
| 36+47, 78 feet LT                          | 160        | 17-      | Install Pole and Anchor                              |
| 36+45, 54 feet LT                          | 165        | 98-12823 | Remove Pole                                          |
| 37+35, 79 feet LT                          | 170        | 17-      | Install Pole and Anchor (Anchor on Private Property) |
| 37+45, 57 feet RT                          | 180        | 98-12856 | Remove Anchor                                        |
| 44+55, 240 feet RT                         | 190        | 17-      | Install new pole and Anchor                          |
| 44+55, 240 feet RT to<br>47+89, 89 feet RT | 190 to 200 | -        | Bore underground cable                               |
| 47+00, 89 feet RT to<br>47+00, 94 feet LT  | 200 to 210 | -        | Bore underground cable                               |
| 47+00, 89 feet RT                          | 200        | -        | Splice Pit                                           |
| 47+00, 94 feet LT                          | 210        | 17U      | Install Pad mount transformer                        |
| 47+00, 94 feet LT to<br>47+60, 113 feet LT | 210 to 220 | -        | Trench Cable                                         |
| 47+60, 113 feet LT                         | 220        | 17-      | Install Pole                                         |
| 47+59, 96 feet LT                          | 225        | 73T40038 | Pole to be Removed by AT&T                           |
| 48+71, 92 feet LT                          | 235        | 68-24394 | Remove pole and anchors                              |

| Station No.                                      | Sequence | Pole No. | Work Proposed                              |
|--------------------------------------------------|----------|----------|--------------------------------------------|
| 51+65, 74 feet LT                                | 245      | 96-05777 | Remove pole and anchors                    |
| 52+37, 165 feet RT                               | 246      | 72-5300  | Pole to Remain                             |
| 58+15, 72 feet RT                                | 250      | 68-24387 | Install anchor 2 ft. deeper for 2 feet cut |
| 106+04, 193 feet LT                              | 260      | 06-05349 | Pole to Remain                             |
| 106+04, 142 feet LT                              | 270      | 17-      | Install Pole                               |
| 106+00, 132 feet RT                              | 280      | 17-      | Install new pole and anchors               |
| 103+89, 120 feet RT                              | 282      | 96-10794 | Relocate light pole to this Station        |
| 103+92, 93 feet RT                               | 282      | 96-10794 | Remove light pole from this Station        |
| 107+13, 90 feet RT                               | 285      | 96-05254 | Remove pole                                |
| 108+00, 146 feet RT                              | 290      | 17-      | Install new pole                           |
| 108+09, 122 feet RT                              | 295      | 09-01673 | Remove pole                                |
| 103+50, 112 feet RT<br>to<br>107+52, 143 feet RT | 292, 294 |          | Place new underground cable                |
| 109+05, 138 feet RT                              | 300      | 96-05271 | Install anchor w/ext. for 2 feet of fill   |

The field contact for WE Energies – Electric is:

Mr. Steven J. King  
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**WE Energies – Gas** has Natural Gas facilities located throughout the length of the project. This work will be performed by WE Energies prior to the start of road construction with utility construction anticipated to begin during late summer/early fall of 2017 and take 90 working days to complete.

| Location and Conflict                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                | Resolution                                                |
|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-----------------------------------------------------------|
| <ul style="list-style-type: none"> <li>• 271+61, LT – service crossing on west side of CTH TT – 6 inch PE</li> <li>• 271+61 – 254+74, LT/RT – north-south direction along west side of CTH TT – 4 inch PE</li> <li>• 263+30, LT – 53+00, RT – east-west direction along south side of Kisdon Hill Drive – 2 inch PE</li> <li>• 254+80, RT – 53+75, RT – east-west direction along south side Madison Street – 2 inch PE</li> <li>• 254+80, RT – 47+60, RT – east-west direction along south side Madison Street – 2 inch PE</li> <li>• 221+00, LT – along south side of Kame Terrace – 4 inch PE</li> <li>• 222+00, LT – 84+00 LT – Merrill Hills Road (CTH TT) – 4 inch PE</li> <li>• 72+60, LT – 65+00, LT – along west side of Merrill Hills Road (CTH</li> </ul> | Facilities planned to be discontinued and remain in place |

|                                                                                                                                                                                                                                                                                                                            |  |
|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|--|
| TT) – 4 inch PE <ul style="list-style-type: none"> <li>• 35+75, LT/RT – crossing STH 59 / Genesee Road</li> <li>• 42+60, LT/RT – crossing STH 59 / Genesee Road</li> <li>• 42+60, LT – 47+25, LT - STH 59 / Genesee Road</li> <li>• 52+50, RT – 57+00, RT – along east side of CTH X / Genesee Road – 6 inch PE</li> </ul> |  |
|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|--|

| Location and Conflict                                                                                                                                                                                                                                                                                                                                                                                                                             | Resolution                                                                       |
|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|----------------------------------------------------------------------------------|
| 4-inch Steel along west Merrill Hills Road at Railroad crossing<br>4-inch PE along west side of Merrill Hills Road north and south at Kame Terrace<br>4-inch PE along south side of Madison Street west of CTH TT<br>6-inch Steel along Madison Street east of CTH TT<br>4-inch PE from Fiddlers Creek Drive south to Madison Street<br>2-inch PE at south side of Kisdon Hill Drive<br>6-inch Steel along south east corner CTH X / Genesee Road | Facilities are no longer in use but have been left in place in the project area. |

| Station                                | Work Proposed                                                   | Offset                                                                                                                                                     |
|----------------------------------------|-----------------------------------------------------------------|------------------------------------------------------------------------------------------------------------------------------------------------------------|
| <b>CTH TT/Proposed Waukesha Bypass</b> |                                                                 |                                                                                                                                                            |
| 271+61 to 253+64                       | 1,566 feet of 6-inch PE Direct Bore                             | North-south direction, with offset varying from 84' LT – 90' LT of R/L                                                                                     |
| 265+25 to 53+00                        | 370 feet of 2-inch PE Direct Bore                               | East-west direction crossing for Kisdon Hill Drive                                                                                                         |
| 254+82 to 47+67                        | 143 feet of 6-inch PE Open Cut                                  | East-west direction connecting to existing line on west leg of Madison Street                                                                              |
| 253+94                                 | 321 feet of 6-inch PE Direct Bore and 218 6-inch PE Direct Bore | East-west direction crossing under CTH TT connecting to existing line on east leg of Madison Street at Station 53+75                                       |
| 232+56 to 234+00                       | 145 feet of 4-inch PE Direct Bore                               | Northeast direction, line offset to accommodate for proposed storm sewer                                                                                   |
| 226+48                                 | 262 feet of 6-inch PE Direct Bore                               | East-west direction crossing under CTH TT, tied into existing line on west side and ending on east side of roadway                                         |
| 226+48 to 84+08                        | 1,210 feet of 6-inch PE Direct Bore                             | North-south direction, with offset varying from 2 feet LT – 6 feet LT of right-of-way                                                                      |
| 84+08                                  | 70 feet of 6-inch PE Direct Bore                                | East-west direction crossing under CTH TT, tied into existing line on west side                                                                            |
| <b>Genesee Road/STH 59</b>             |                                                                 |                                                                                                                                                            |
| 33+33 to 47+25                         | 1,428 feet of 6-inch PE Direct Bore                             | East-west direction, with offset varying from 2 feet RT – 10 feet RT of right-of-way; line connecting into existing line on west side of Valley View Drive |
| 52+62 to 57+00                         | 429 feet of 8-inch PE Direct Bore                               | East-west direction connecting to existing line, parallel offset of discontinued line                                                                      |
| Note: PE = Polyethylene pipe           |                                                                 |                                                                                                                                                            |

Relocations and adjustments of WE Energies Gas facilities are anticipated to commence prior to the start of this roadway project.

It is imperative that the highway contractor contact WE Energies before removing any gas facilities or electrical underground cables, to verify that they have been discontinued and carry no natural gas or electrical current. The contractor must not assume that unmarked facilities have been discontinued. At no time is it acceptable to push, pull, cut or drill an unmarked facility without explicit consent from We Energies.

Contractor shall contact We Energies gas dispatch at (800) 261-5325 or We Energies electric dispatch at (800) 662-4797 at least 24 hours prior to removing any discontinued gas facilities or electrical cable to verify that they have been discontinued and carry no natural gas or electrical current. There may be locations where existing or discontinued utilities are not shown on the plans.

**Contractor must call the We Energies 24-hour Dispatch lines to arrange for this verification.**  
**We Energies Electric Dispatch #1-800-662-4797**  
**We Energies Gas Dispatch #1-800-261-5325**

The construction field contact for We Energies – Gas is:

Ms. Danielle Fink  
500 S. 116th Street  
West Allis, WI 53214  
(414) 651-3067  
[danielle.fink@we-energies.com](mailto:danielle.fink@we-energies.com)

**Windstream - OSP Network (Communication)** has aerial facilities that will require relocation and new installations.

New aerial fiber will be placed on the new WE Energies poles from Station 264+32, 90 feet RT, following the new WE Energies power pole placement route to the new WE Energies pole located at Station 83+76, 25 feet RT (existing Merrill Hills Rd). From the new WE Energies pole at Station 83+76, 25 feet RT, the existing aerial fiber will have attachments transferred to new WE Energies poles between Stations 177+52, 85 feet RT to 62+44, 34 feet LT (existing Merrill Hills Rd.).

Windstream relocation work is anticipated to last 45 working days and will begin after Charter has completed their installations

Windstream's existing underground fiber which exists north of the existing Sunset drive pavement (from Station 47+00 to Station 61+50), will be relocated. New underground fiber will be installed from station 47+00, extending north to within 2 feet of the north R/W of Sunset Drive (Station 47+00, 47 feet LT. Underground line will continue easterly along the north R/W to Station 61+50, 48 feet LT , where it will turn south crossing under Sunset Drive at Station 61+50. The fiber will rise up the new We Energies pole at Station 61+58, 48 feet RT where it will be spliced into the relocated aerial fiber which continues to the east. The existing underground fiber will be discontinued in place.

Windstream relocation work is anticipated to last 28 days and will begin after We energies has completed their pole installations.

The construction field contact for Windstream is:

Ms. Mary Beth Fisher  
Project Manager – OSP Network  
13935 Bishops Dr.  
Brookfield, WI 53005

Work: (262) 792-7938  
Cell: (414) 313-9032  
[Mary.B.Fisher@windstream.com](mailto:Mary.B.Fisher@windstream.com)

**Wisconsin Department of Transportation - Traffic Signals** – has underground and above ground signal facilities at the intersection of STH 59 and CTH X. The traffic signals will be removed as part of this construction project.

| Location and Conflict                                                                                                                                                                                                                                | Resolution              |
|------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-------------------------|
| <ul style="list-style-type: none"><li>• CTH X (Saylesville) and STH 59 (Genesee Road)</li><li>• STH 59 and CTH X (Genesee Road)</li><li>• USH 18 / Meadowbrook and CTH D (Sunset Drive)</li><li>• USH 18 / Meadowbrook and Madison Street.</li></ul> | Signalized intersection |

The construction field contacts for WisDOT Traffic Signal Operations are:

Mr. David Brantner  
141 NW Barstow St.  
Waukesha, WI 53187  
[david.brantner@dot.wi.gov](mailto:david.brantner@dot.wi.gov)

WisDOT Electric Field Unit  
935 S. 60th St.  
West Allis, WI 53214  
(414) 266-1170

## 11. Other Contracts.

*Replace section titled **West Waukesha Bypass North** with the following:*

### **West Waukesha Bypass North**

WisDOT and Waukesha County, Wisconsin have a project north of this contract. Work under this contract Project 2788-02-70 and 2788-00-72 was LET in February of 2017. This project connects to this contract at the northern limit, Station 273+75. Project 2788-02-70 and 2788-00-72 are anticipated to be completed early August 2018. Traffic switches must be coordinated between both projects. Coordinate activities with the project contractor and engineer. The WisDOT engineer is:

John Kanzenbach, (262) 548-6467, [John.Kanzenbach@dot.wi.gov](mailto:John.Kanzenbach@dot.wi.gov)

*Replace section titled **Wisconsin and Southern Railroad LLC Southern Siding Track Relocation** with the following:*

### **Wisconsin and Southern Railroad LLC Southern Siding Track Relocation**

WSOR will complete the southern Siding track removal. Coordinate construction activities with the WSOR representative and engineer. The WSOR contact is:

Roger Schaalma, Superintendent of Maintenance of Way, Wisconsin and Southern Railroad LLC, 1890 East Johnson Street, Madison, WI 53704; Telephone: (608) 620-2044; Ext. 4201; email [rschaalma@watcocompanies.com](mailto:rschaalma@watcocompanies.com) for consultation on railroad requirements during construction.

*Add the following section:*



### **Signal Interconnect Project (West Waukesha Bypass)**

A signal interconnect project will be built between USH 18 and STH 59 in August 2019. Conduit will be placed in parapet of structures spanning the project length. Work will be done under separate project. Coordinate construction activities with David Brantner, (262) 548-8736, david.brantner@dot.wi.gov

## **12. Railroad Insurance and Coordination – Wisconsin and Southern Railroad LLC.**

*Replace entire section titled **A.1 Railroad Insurance Requirements** with the following:*

### **A.1 Railroad Insurance Requirements**

In addition to standard spec 107.26, provide railroad protective liability insurance coverage as specified in standard spec 107.17.3. Insurance is filed in the name of Wisconsin and Southern Railroad LLC.

Notify evidence of the required coverage, and duration to Amanda Haggerty, Office Administrator at Wisconsin and Southern Railroad LLC, 1890 East Johnson Street, Madison, WI 53704, telephone (608) 620-2048, email ahaggerty@watcocompanies.com.

Also send a copy to the following: Paul Derksen, SE Region Railroad Coordinator; 141 N. W. Barstow Street, Waukesha, WI 53188; Telephone (262) 548-8770; E-mail: paul.derksen@dot.wi.gov.

Include the following information on the insurance document:

Project 2788-00-71

Project Location: Town of Waukesha, WI

Route Name CTH TT, Waukesha County

Crossing ID 391 530S

Railroad Subdivision Waukesha

Railroad Milepost 22.68

Work Performed: EBS excavation, select crushed material, excavation, hauling, and disposal of petroleum contaminated soil, structure excavation, grading, base course and pavement.

*Replace entire section titled **A.4 Work by Railroad** with the following:*

### **A.4 Work by Railroad**

The railroad will perform the work described in this section, except for work described in other special provisions and will be accomplished without cost to the contractor. Coordinate construction activities with the WSOR representative and engineer.

Prior to Stage 2E, WSOR to remove the southerly siding track.

During Stage 2E, the new crossing surface will be completed for the Bypass. The railroad will remove the existing ties, rail and crossing surface and install new ties, rail and crossing surface at the new Bypass location. Both southbound temporary and permanent railroad warning devices are to be installed.

During Stage 3 the new southbound Bypass roadway will carry two-way traffic, requiring a temporary signal for northbound traffic. Prior to Stage 4 the permanent northbound warning devices will be installed when grading is complete at the signal locations.

During Stage 4, the temporary railroad warning devices will be removed.

Amend standard spec 108.4 to include the railroad in the distribution of the initial bar chart, and monthly schedule updates. The bar chart shall specifically show work involving coordination with the railroad.

**139. Seismograph, Item 999.1000.S.**

**A Description**

This special provision describes furnishing a seismograph(s) and employing trained operators to monitor construction-induced vibrations on buildings/structures, and submittal of all required documentation.

**B Material**

Use seismographs conforming to Wisconsin Department of Safety and Professional Services (SPS) 307.43, Wisconsin Administrative Code that are continuous data recorders supplied with all the accessories necessary for making vibration and noise monitoring observations.

**C Construction**

Conduct monitoring procedures conforming to SPS 307.44 and as follows: Take seismograph readings prior to construction activities to establish an ambient or background index.

During construction, place seismograph to monitor all vibration-inducing construction activities or as directed by the engineer. At a minimum utilize one seismograph. If more than one major construction activity per day is taking place, multiple seismographs may be required. Place the seismograph on a stable surface within 3 feet of the building/structure nearest to the construction operation. Provide data recorded for each vibration occurrence to the engineer which includes the following:

1. Identification of vibration monitoring instrument used.
2. Description of equipment used by the contractor.
3. Name of qualified observer and interpreter.
4. Distance and direction of recording station from the vibration area.
5. Type of ground at recording station and material on which the instrument is sitting.
6. Peak particle velocity and principal frequency in each component.
7. A dated and signed copy of records of seismograph readings.
8. A comparison of measured seismograph readings to maximum allowable readings identified in SPS 307.43 or as specified in this special provision.

If construction activities generate ground vibration in excess of the peak particle velocity limits as shown in SPS 307.44, stop the construction operation in progress and implement alternate construction methods to produce results within the allowable peak particle velocity limits.

**D Measurement**

The department will measure Seismograph as a single complete lump sum unit of work, acceptably completed.

**E Payment**

The department will pay for measured quantities at the contract unit price under the following bid item:

| ITEM NUMBER | DESCRIPTION | UNIT |
|-------------|-------------|------|
| 999.1000.S  | Seismograph | LS   |

Payment is full compensation for furnishing and operating a seismograph(s), any operator(s), and for producing documentation reports  
stp-999-005 (20161130)

**140. Wall R-67-145 Fence Chain Link Polymer Coated 4-FT, Item SPV.0090.06; Fence Chain Link Polymer Coated 4-FT, Item SPV.0090.07.**

**A Description**

This special provision describes furnishing and installing new black vinyl clad chain link fencing according to standard spec 616, as directed by the engineer and as hereinafter provided.

**B Materials**

Conform to standard spec 616.2 and as herein after provided.

Provide chain link fence with a polymer coating and conforming to AASHTO M181. Provide fabric woven of 9-gage wire in 2-inch diamond pattern mesh with both the top and bottom selvages knuckled. Also provide polymer coated ties and tension bars conforming to AASHTO M181. Ensure that the color of all fencing components is black.

**C Construction**

Install chain link fence fabric conforming to standard spec 616.3.3.3 and the plan details. For Wall R-67-145 Fence Chain Link Polymer Coated 4-FT, each post is to be supported by concrete footing as show in the plans.

Touch up painted framework surfaces marred by fencing installation.

**D Measurement**

The department will measure Wall R-67-145 Fence Chain Link Vinyl Coated 4-FT and Fence Chain Link Vinyl Coated 4-FT by the linear foot, acceptably completed.

**E Payment**

The department will pay for measured quantities at the contract unit price under the following bid item:

| ITEM NUMBER | DESCRIPTION                                        | UNIT |
|-------------|----------------------------------------------------|------|
| SPV.0090.06 | Wall R-67-145 Chain Link Fence Polymer Coated 4-FT | LF   |
| SPV.0090.07 | Chain Link Fence Polymer Coated 4-FT               | LF   |

Payment is full compensation for clearing and grubbing the fence line; for excavating; for transporting, setting posts including placing concrete, for erecting and tensioning all fencing components; for installing grounds; for painting the framework, including touch up; for removing and disposing all debris, and for access excavation and surplus materials.

**141. Temporary Precast Trench Drain, Item SPV.0090.08.**

**A Description**

This special provision describes providing a precast trench drain system and temporary precast trench drain systems as the plans show. Conform to standard spec 415 and 611 and as follows.

**B Materials**

**B.1 Documentation**

Submit manufacturer's specifications, certifications, and installation instructions for grates, frames,

connections, and precast drain channel two weeks before placement for engineer approval.

## **B.2 Frames and Grates**

Furnish frames and grates manufactured of ductile iron conforming to ASTM A536 and meets AASHTO HS-25 load ratings for heavy duty high speed traffic. Ensure that frames and grates are one piece anchored into the body of the line drain, except where the plans show removable grates.

For removable grates, provide a repetitive minimum pullout resistance of 340 pounds per foot of length after completion of 1,000 hours of salt spray testing according to ASTM B117. Match removable grates to their frames in pairs before delivery to the worksite. Ensure that grates fit into frames without rocking. Furnish corrosion resistant locking devices for removable grates.

Secure the trench drain system in concrete according to the manufacturers specifications. Use concrete conforming to standard spec 415.2.1.

Furnish concrete curing compounds conforming to standard spec 415.2.4.

## **B.3 Precast Drain Channel**

Furnish precast drain channel sections constructed of monolithic polymer concrete. Ensure that the interior surface of the channel is smooth below the level of the frame, grate, and associated connections. Use polymer concrete consisting of aggregate with either polyester resin or vinylester resin. Ensure that the polymer concrete conforms to the following:

| <b>Property</b>      | <b>ASTM Test Method</b> | <b>VALUE</b>                             |
|----------------------|-------------------------|------------------------------------------|
| Compressive Strength | C-579                   | 12,000 psi minimum                       |
| Tensile Strength     | C-307                   | 1,500 psi minimum                        |
| Flexural Strength    | C-580                   | 3,000 psi minimum                        |
| Moisture Absorption  | C-140                   | 5% max                                   |
| Chemical Resistance  | C-267                   | Pass                                     |
| Freeze Thaw          | C-666                   | 1,600 minimum cycles without weight loss |

## **C Construction**

Excavate trench channel to the lines and grades the plans show. Grade and compact the bottom of the trench to provide firm and uniform bearing throughout.

Install the trench drain channel, trench drain joints, and connections according to manufacturers' instructions. Install trench drains to the lines and grades the plans show. Securely join sections of the precast drain channel to prevent separation during backfilling. Connect to existing drainage facilities as the plans show before placing concrete backfill.

Place concrete in channel without floating or shifting the line drain and without concrete segregation. Secure the frames or the line drain wall into the concrete with steel anchoring rods. Ensure that concrete backfill is flush with the adjacent surfaces and with the drain's frame. Texture the surface of the concrete with a broom or burlap to produce a durable, skid-resistant surface.

## **D Measurement**

The department will measure Precast Trench Drain and Temporary Precast Trench Drain by the linear foot acceptably completed.

## **E Payment**

The department will pay for measured quantities at the contract unit price under the following bid item:

ITEM NUMBER  
SPV.0090.08

DESCRIPTION  
Temporary Precast Trench Drain

UNIT  
LF

Payment is full compensation for providing precast trench drain; for excavation; aggregate base materials; for concrete backfill; and for disposing of waste materials and restoring the site.

SER-611-002 (20180327)

**Schedule of Items**

Attached, dated July 2, 2018, are the revised Schedule of Items Pages 1, 10, 12, 13, 19, 23, 32, and 36.

**Plan Sheets**

The following 8½ x 11-inch sheets are attached and made part of the plans for this proposal:

Revised: 4, 43, 60, 62, 223, 457, 481, 554, 557, 558, 565, 570, 599, 610, 642, 694, 695, 697, 769, 794, 800, 837, 840, 903, 1280, 1281, 1290, 1320, 1321, 1331, 1359, 1360, 1370, 1406, 1407, 1418-1420, 1436, 1440, 1462-1583

Added: 43A, 1297A, 1337A, 1379A, 1432A

END OF ADDENDUM

**AT&T CORPORATION (LONG DISTANCE NETWORK)**

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**MCI**

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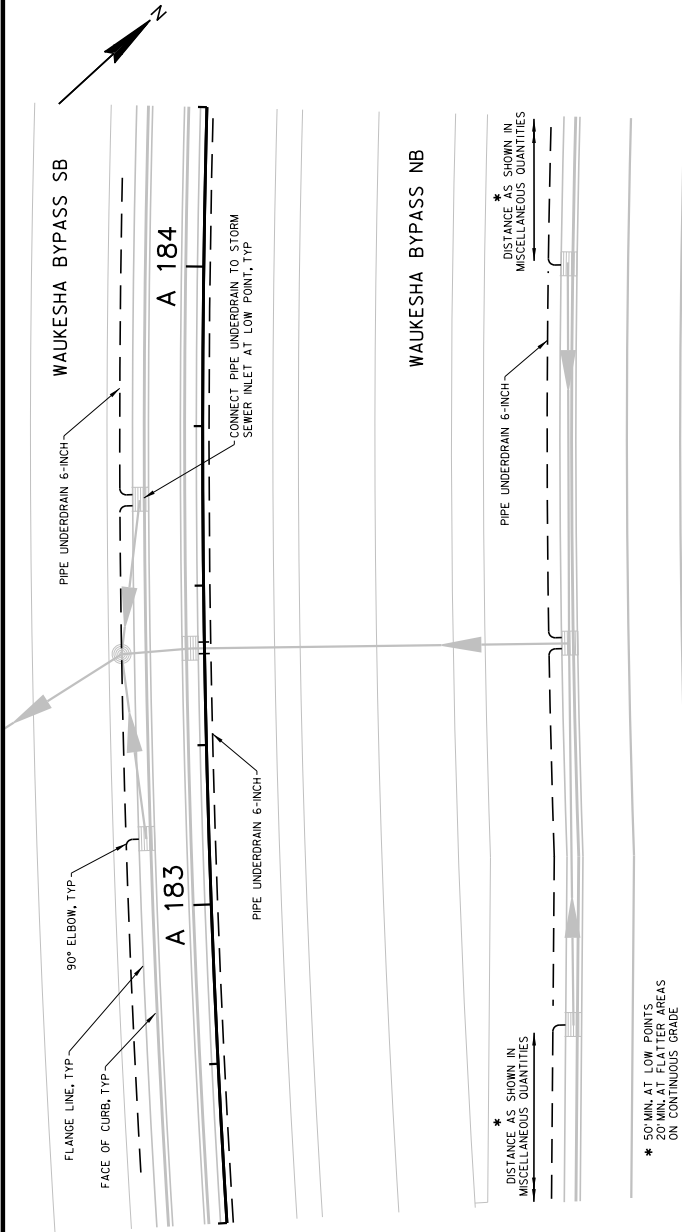
**TELEPORT COMMUNICATIONS**

AMERICA, LLC  
MS. JENNIFER NAVARRO  
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**WAUKESHA COUNTY DPW - TRAFFIC SIGNALS**

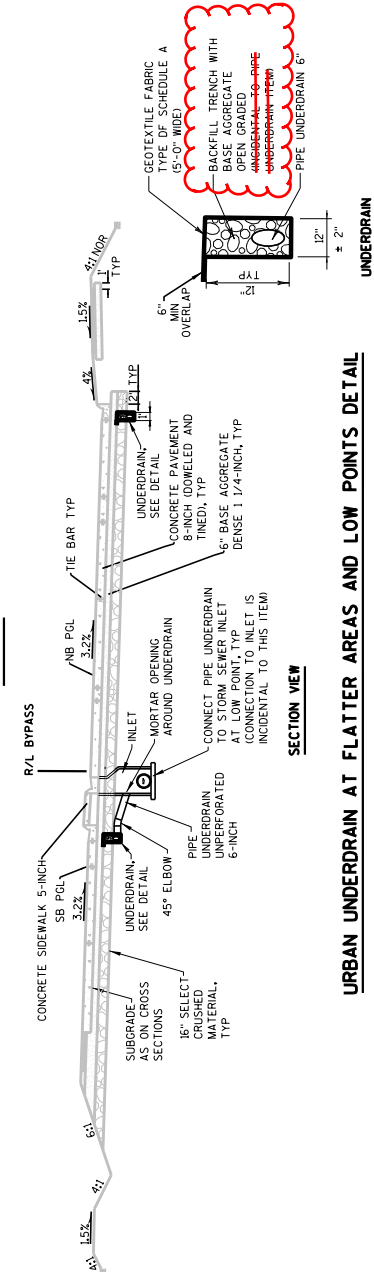
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Addendum No. 01  
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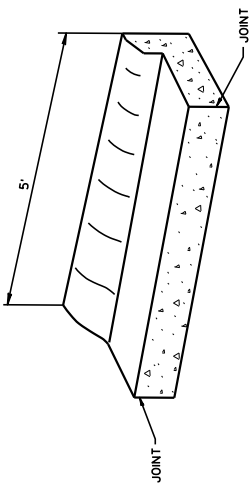
PLAN VIEW

SECTION VIEW



URBAN UNDERDRAIN AT FLATTER AREAS AND LOW POINTS DETAIL

| Waukesha Bypass |           | Waukesha Bypass |                 | Genesee Rd  |             | Saylesville Rd |             |
|-----------------|-----------|-----------------|-----------------|-------------|-------------|----------------|-------------|
| 100+56 NB/SB    | 125+33 SB | 158+82 SB       | 190+19 SB       | 40+19 EB    | 13+22 EB    | 13+22 EB       | 13+22 EB    |
| 107+07 NB       | 137+00 NB | 168+28 SB       | 198+00 NB       | 49+00 EB    | 13+22 EB    | 13+22 EB       | 13+22 EB    |
| 113+43 NB       | 142+15 SB | 171+25 NB       | 201+00 NB       | 49+00 EB    | 17+30 NB    | 17+30 NB       | 17+30 NB    |
| 114+62 NB       | 146+43 SB | 172+50 NB       | 217+63 SB       | 52+43 EB    | 17+36 EB    | 17+36 EB       | 17+36 EB    |
| 114+75 SB       | 148+54 SB | 175+81 NB       | 217+63 SB/NB RT | 54+60 WB    | 17+95 EB/WB | 17+95 EB/WB    | 17+95 EB/WB |
| 121+51 SB       | 155+40 SB | 182+00 NB       | 220+00 SB       | 59+69 EB/WB |             |                |             |

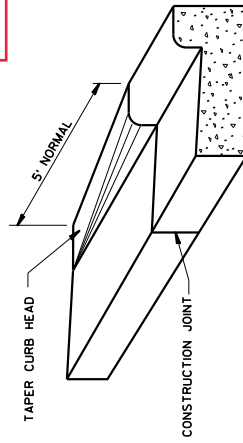


NOTE

D TRANSITIONS ARE BETWEEN CONCRETE CURB & GUTTER 6-INCH SLOPED 3/8-INCH TYPE D TO CONCRETE CURB & GUTTER 3/8-INCH TYPE D.

**CURB & GUTTER TRANSITION DETAIL**

(TO BE MEASURED AND PAID FOR AS CONCRETE CURB & GUTTER 6-INCH SLOPED 3/8-INCH TYPE D)



NOTE

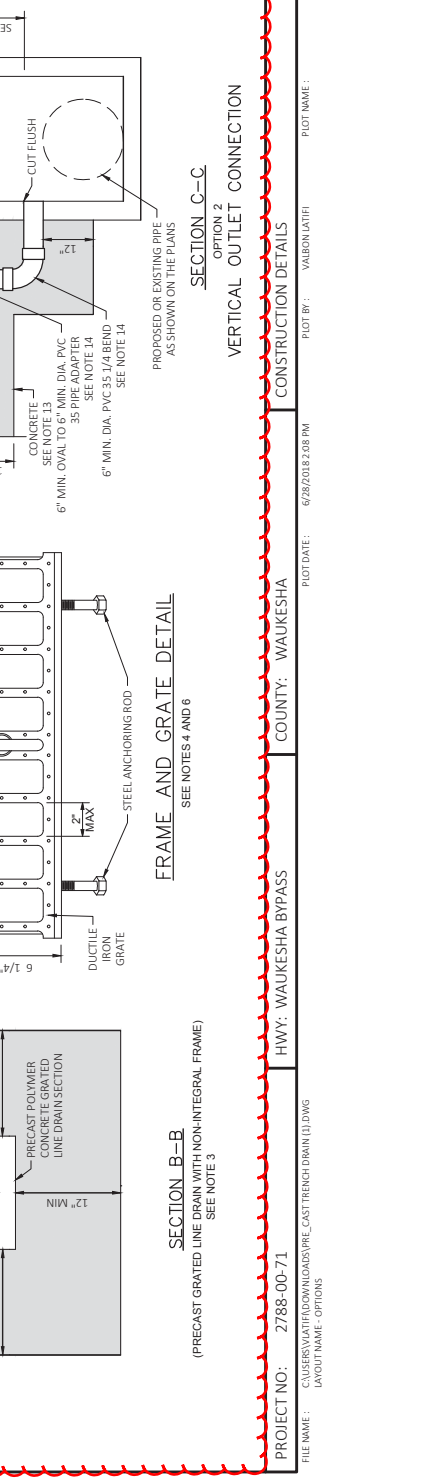
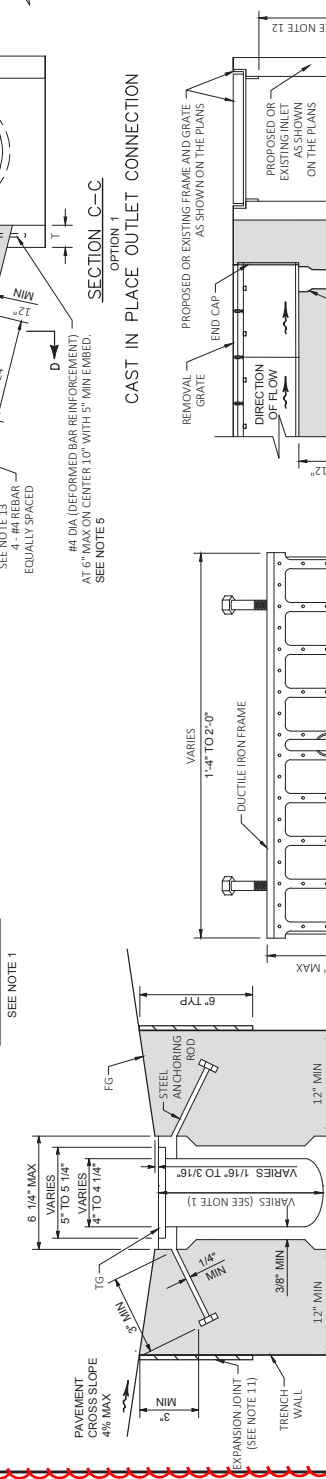
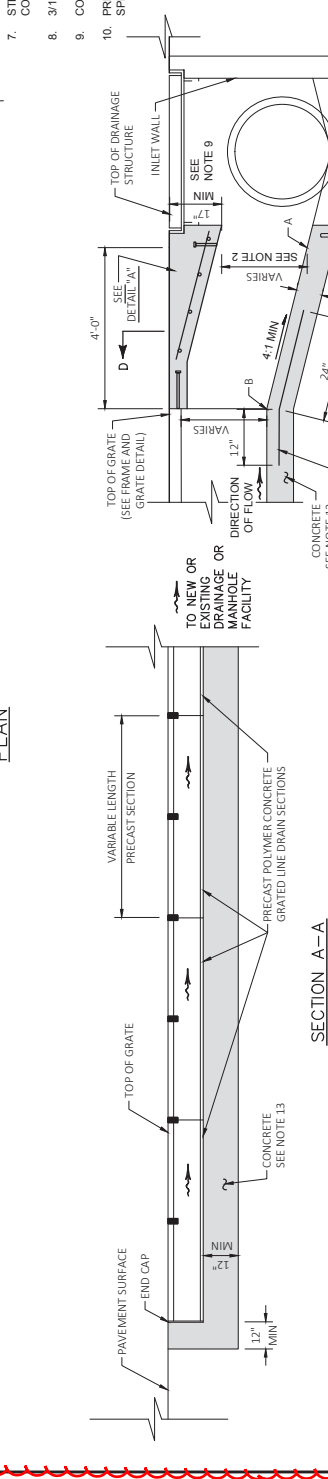
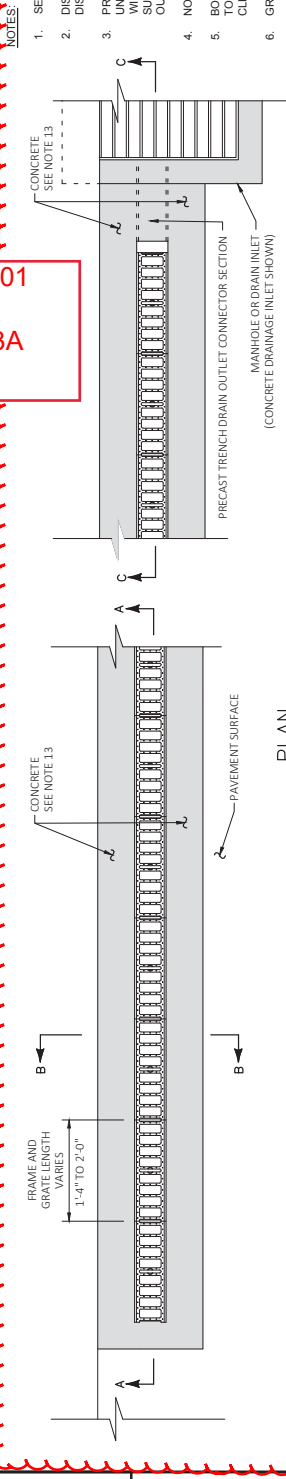
D TO BE USED AT ALL TERMINI OF CURB & GUTTER WHERE NO ABUTTING EXISTING CURB & GUTTER OR CONCRETE SURFACE DRAINS..

**CURB & GUTTER TERMINAL SECTION DETAIL**

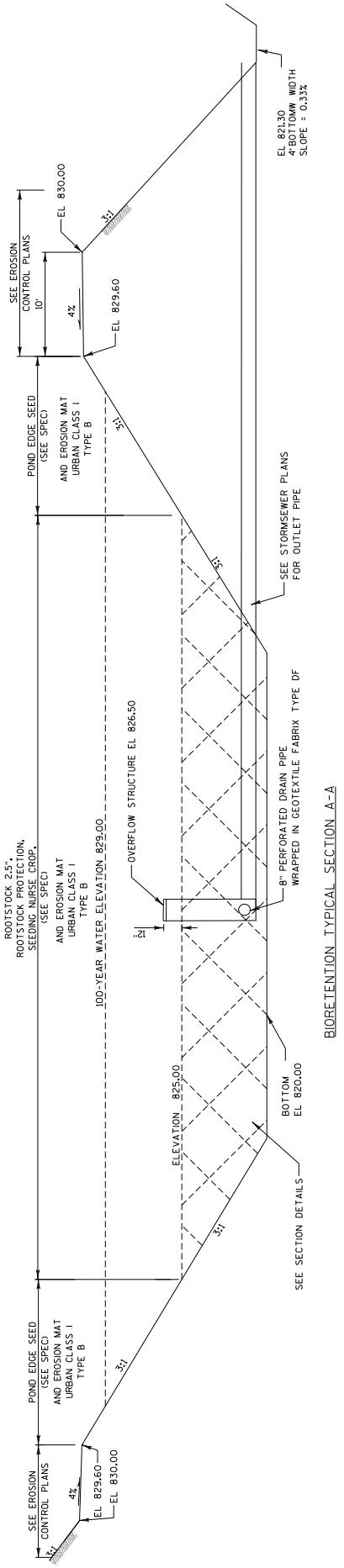
Addendum No. 01  
ID 2788-00-71  
Revised Sheet 43  
July 2, 2018

**Addendum No. 01**  
**ID 2788-00-71**  
**Added Sheet 43A**  
**July 2, 2018**

- NOTES:**
1. SEE QUANTITY SHEETS FOR DISCHARGE CAPACITY REQUIREMENTS.
  2. DISCHARGE CAPACITY (GFS) AT POINT A MUST BE EQUIVALENT TO MAXIMUM CHANNEL DISCHARGE CAPACITY OF GRATED LINE DRAIN AT POINT B.
  3. PRECAST POLYMER CONCRETE DRAIN SECTIONS ARE AVAILABLE IN NON-SLOPED UNIFORM DEPTH SECTIONS 4 7/16" TO 12" OR IN 0.6 PERCENT PRE-SLOPED SECTIONS WITH SLOPE DRAININGS FOR PRECAST TRENCH DRAIN SECTIONS SHOWING ALL OUTLET CONNECTIONS AND GRADES.
  4. NOMINAL DIMENSIONS SHOWN. ALLOWABLE TOLERANCE +/- 2%.
  5. BOTTOM ROW OF DOWELS TO MATCH INLET CONNECTION SLOPE WITH 1/2" MIN CLEAR TO INSIDE OF BOX. (WHEN TPO USE 4 1/2" MIN EMBED)
  6. GRATE PATTERNS MAY VARY FROM DETAIL SHOWN.
  7. STEEL ANCHORING RODS NOT USED WHEN FRAME IS INTEGRAL WITH POLYMER CONCRETE GRATED LINE DRAIN SECTION.
  8. 3/16" MAXIMUM GAP BETWEEN ADJACENT GRATINGS.
  9. CONTRACTOR TO FIELD VERIFY MINIMUM DEPTH TO AVOID CONFLICT WITH INLET TOP.
  10. PRECAST TRENCH DRAINS SHALL BE IN ACCORDANCE WITH MANUFACTURERS SPECIFICATIONS AND DETAILS.
  11. PROVIDE A 1/4"-1/2" EXPANSION JOINT WITH AN EMLIFIED JOINT FILLER BETWEEN CONCRETE PAVEMENT AND TRENCH DRAIN ENCASEMENT.
  12. CONTRACTOR TO FIELD VERIFY MINIMUM DEPTH TO AVOID CONFLICT WITH CASTINGS, RINGS OR STRUCTURE DECK.
  13. PAVEMENT GRADE SHALL MATCH ABUTTING MAINLINE CONCRETE PAVEMENT, UNLESS OTHERWISE APPROVED BY THE ENGINEER.
  14. SIZE OF PIPE TO BE DETERMINED BY CONTRACTOR BASED ON HYDRAULIC CAPACITY, 6" MINIMUM

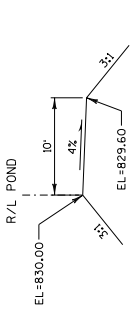






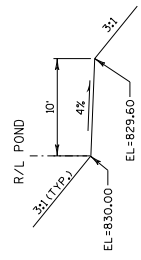
BIORETENTION TYPICAL SECTION A-A

Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 60  
 July 2, 2018



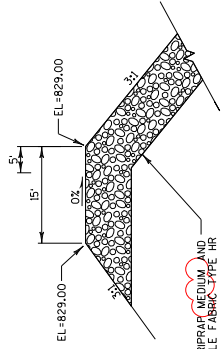
TYPICAL SECTION ACCESS ROAD

STA 20+00 TO STA 22+40; STA 23+64 TO STA 21+24



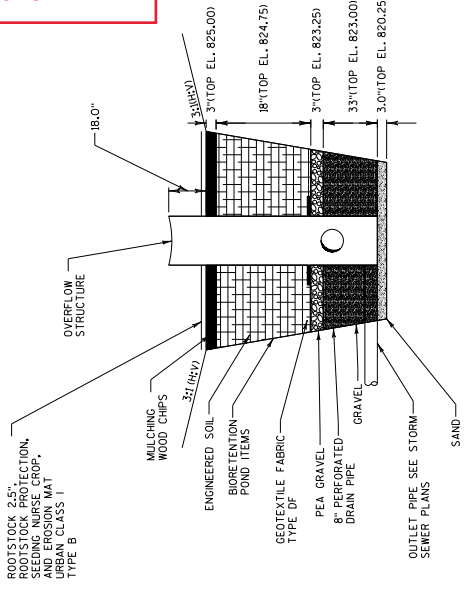
TYPICAL SECTION POND ACCESS ROAD

STA 22+40 TO STA 23+64

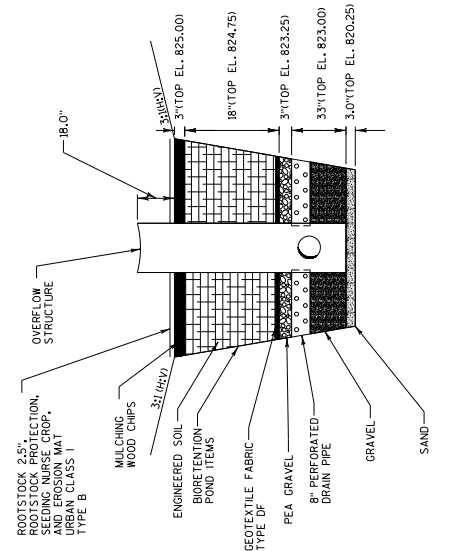


TYPICAL SECTION POND ACCESS ROAD - EMERGENCY SPILLWAY

STA 21+50 TO STA 21+70

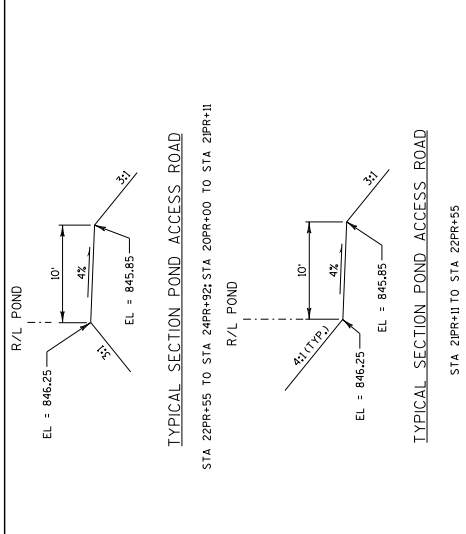
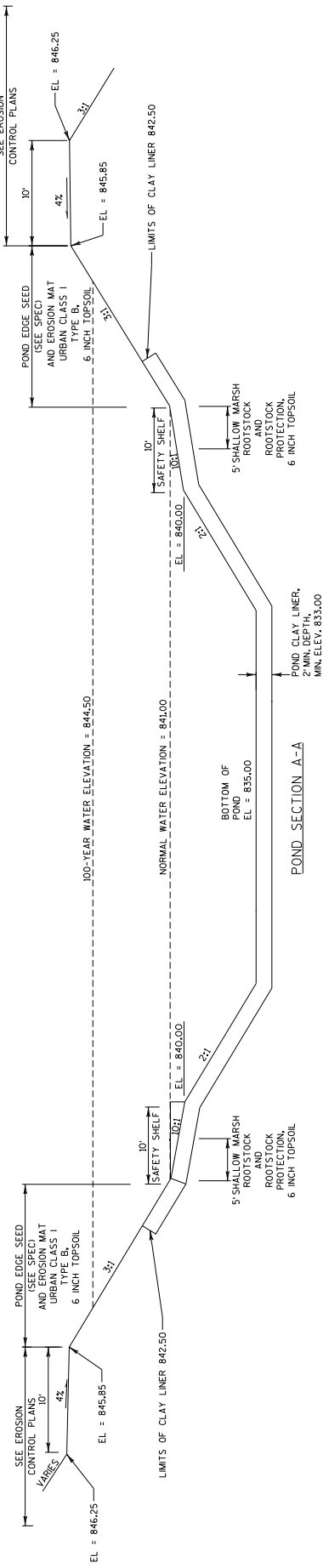


BIORETENTION SECTION C-C

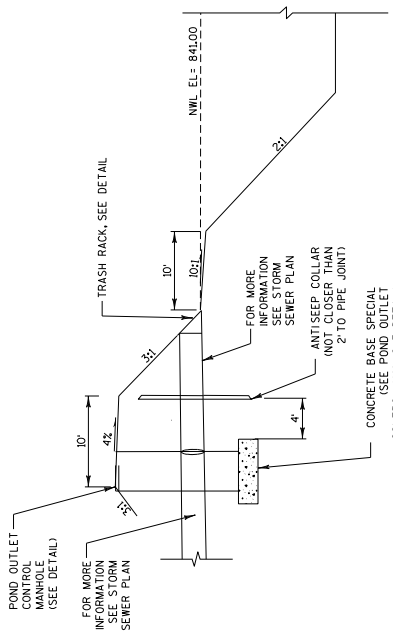


BIORETENTION SECTION B-B

|                                                           |                       |                                                          |          |   |
|-----------------------------------------------------------|-----------------------|----------------------------------------------------------|----------|---|
| PROJECT NO: 2788-00-71                                    | COUNTY: WAUKESHA      | STORMWATER MANAGEMENT - BIORETENTION CONSTRUCTION DETAIL | SHEET 60 | E |
| HWY: WAUKESHA BYPASS                                      | PLOT BY: Pw.L.P.E.I.m | PLOT SCALE: 50:1                                         |          |   |
| FILE NAME: \\m14001rpg\proj\59971\1\cads\by_022902-sw.dgn | PLOT DATE: 6/28/2018  | WISDOT/CADD SHEET 42                                     |          |   |



Addendum No. 01  
ID 2788-00-71  
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July 2, 2018

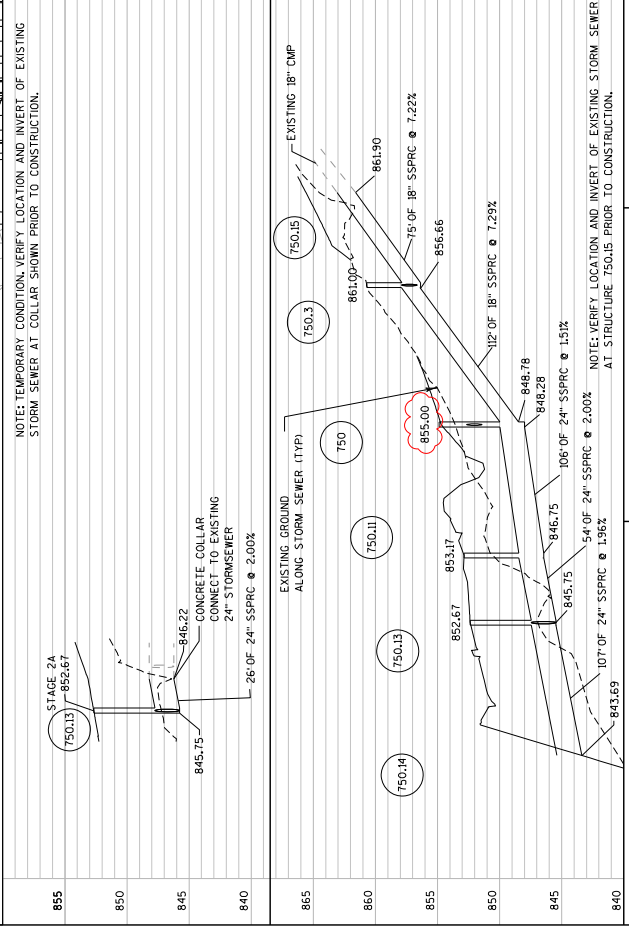
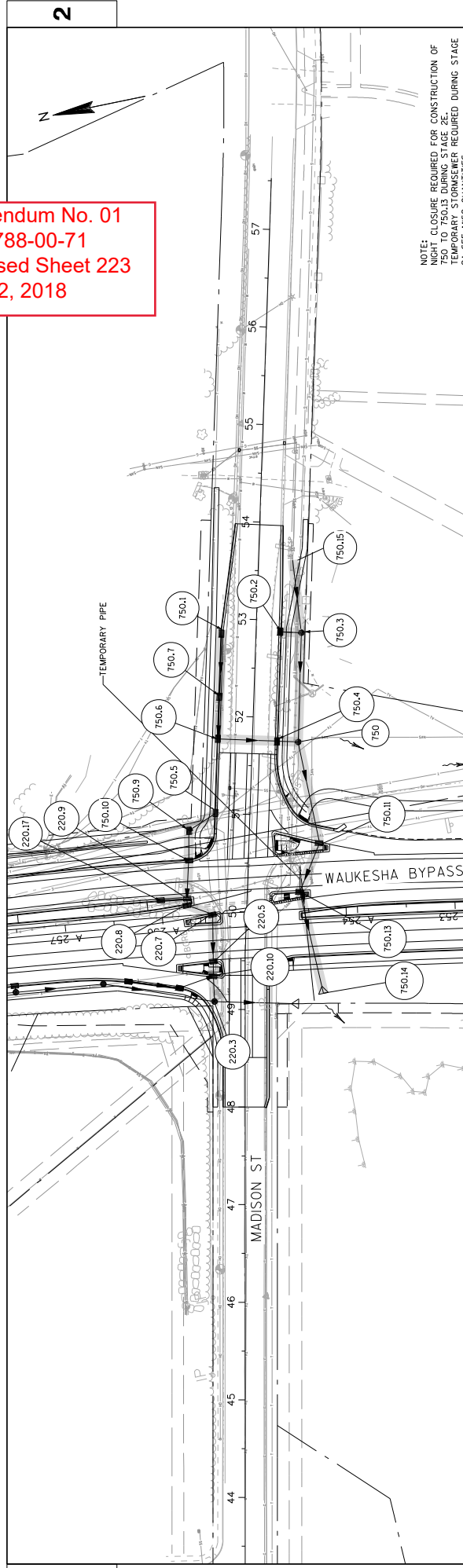


- NOTES
1. REFER TO STANDARD DETAIL CATCH BASIN 6-FT DIAMETER FOR OTHER INFORMATION.
  2. OUTLET STRUCTURE WILL BE PAID FOR AS POND OUTLET CONTROL MANHOLE.
  3. ALL STRUCTURES TO BE FINISHED OR SIMILARLY PROTECTED FROM CORROSION.
  4. CONNECTOR DEVICE WILL BE REMOVABLE AND APPROVED BY THE ENGINEER.



|                                                               |                       |                                                  |                   |
|---------------------------------------------------------------|-----------------------|--------------------------------------------------|-------------------|
| PROJECT NO: 2788-00-71                                        | COUNTY: WAUKESHA      | STORMWATER MANAGEMENT - POND CONSTRUCTION DETAIL | SHEET 62          |
| FILE NAME : \\m1.lw00.vrnp\proj\59971\1\cadd\by_022904_sw.dgn | PLOT DATE : 6/28/2018 | PLOT NAME : \$FILES                              | PLOT SCALE : 50:1 |
| HWY: WAUKESHA BYPASS                                          |                       | WISDOT/CADD SHEET 42                             |                   |

Addendum No. 01  
ID 2788-00-71  
Revised Sheet 223  
July 2, 2018



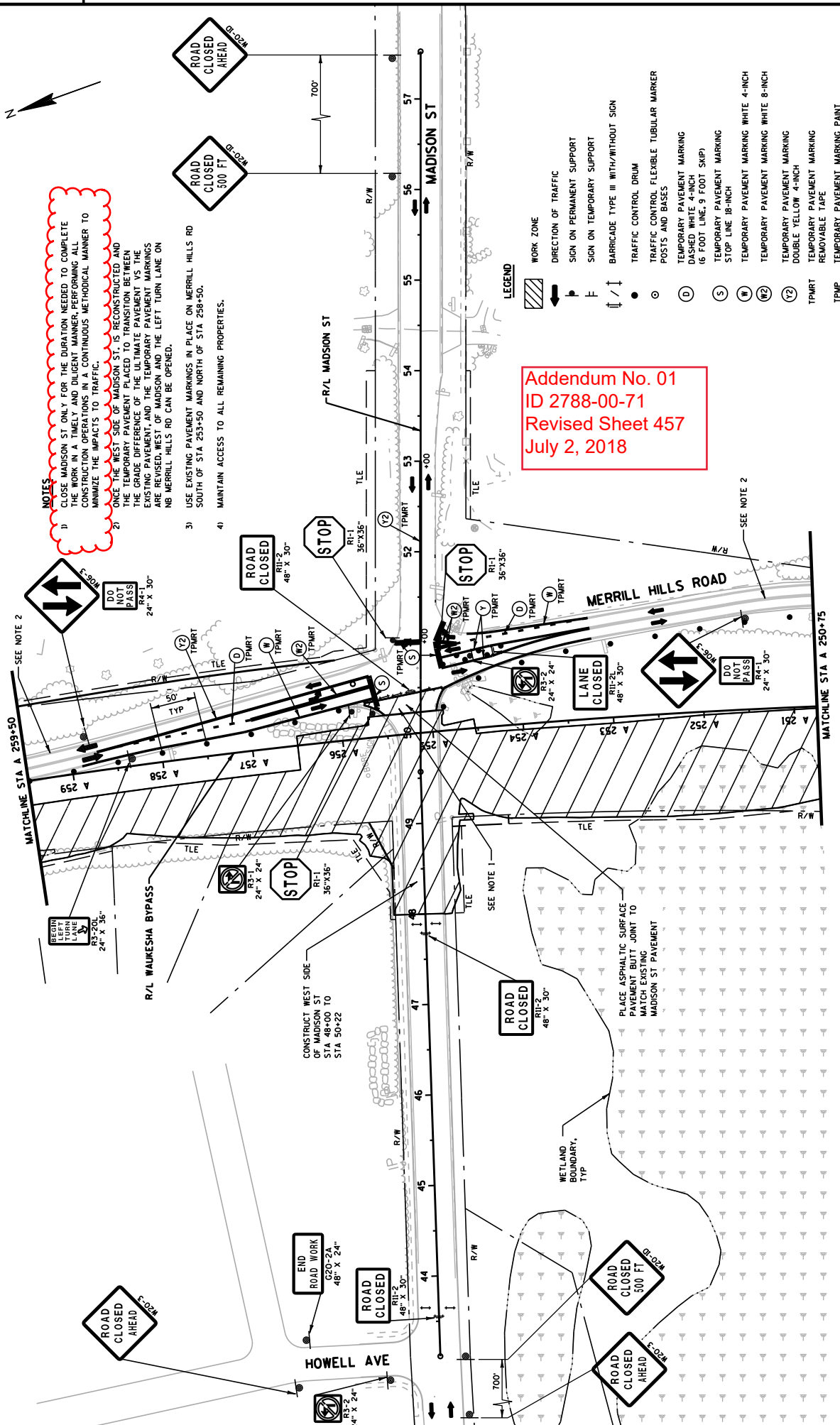
NOTE: CLOSURE REQUIRED FOR CONSTRUCTION OF HIGHWAY OVER EXISTING STORM SEWER. TEMPORARY STORMSEWER REQUIRED DURING STAGE 2A-SEE MISC. QUANTITIES.

|     |     |     |     |
|-----|-----|-----|-----|
| 855 | 860 | 860 | 860 |
| 850 | 855 | 855 | 855 |
| 845 | 850 | 850 | 850 |
| 840 | 845 | 845 | 845 |
| 865 | 865 | 865 | 865 |
| 860 | 860 | 860 | 860 |
| 855 | 855 | 855 | 855 |
| 850 | 850 | 850 | 850 |
| 845 | 845 | 845 | 845 |



**NOTES:**

- 1) CLOSE MADISON ST ONLY FOR THE DURATION NEEDED TO COMPLETE THE WORK IN A TIMELY AND DILIGENT MANNER, PERFORMING ALL CONSTRUCTION OPERATIONS IN A CONTINUOUS METHODOICAL MANNER TO MINIMIZE THE IMPACTS TO TRAFFIC.
- 2) ONCE THE WEST SIDE OF MADISON ST IS RECONSTRUCTED AND THE TEMPORARY PAVEMENT PLACED TO TRANSITION BETWEEN THE GRADE DIFFERENCE OF THE ULTIMATE PAVEMENT VS THE EXISTING PAVEMENT, AND THE TEMPORARY PAVEMENT MARKINGS ARE REVISED, WEST OF MADISON AND THE LEFT TURN LANE ON NB MERRILL HILLS RD CAN BE OPENED.
- 3) USE EXISTING PAVEMENT MARKINGS IN PLACE ON MERRILL HILLS RD SOUTH OF STA 253+50 AND NORTH OF STA 258+50.
- 4) MAINTAIN ACCESS TO ALL REMAINING PROPERTIES.

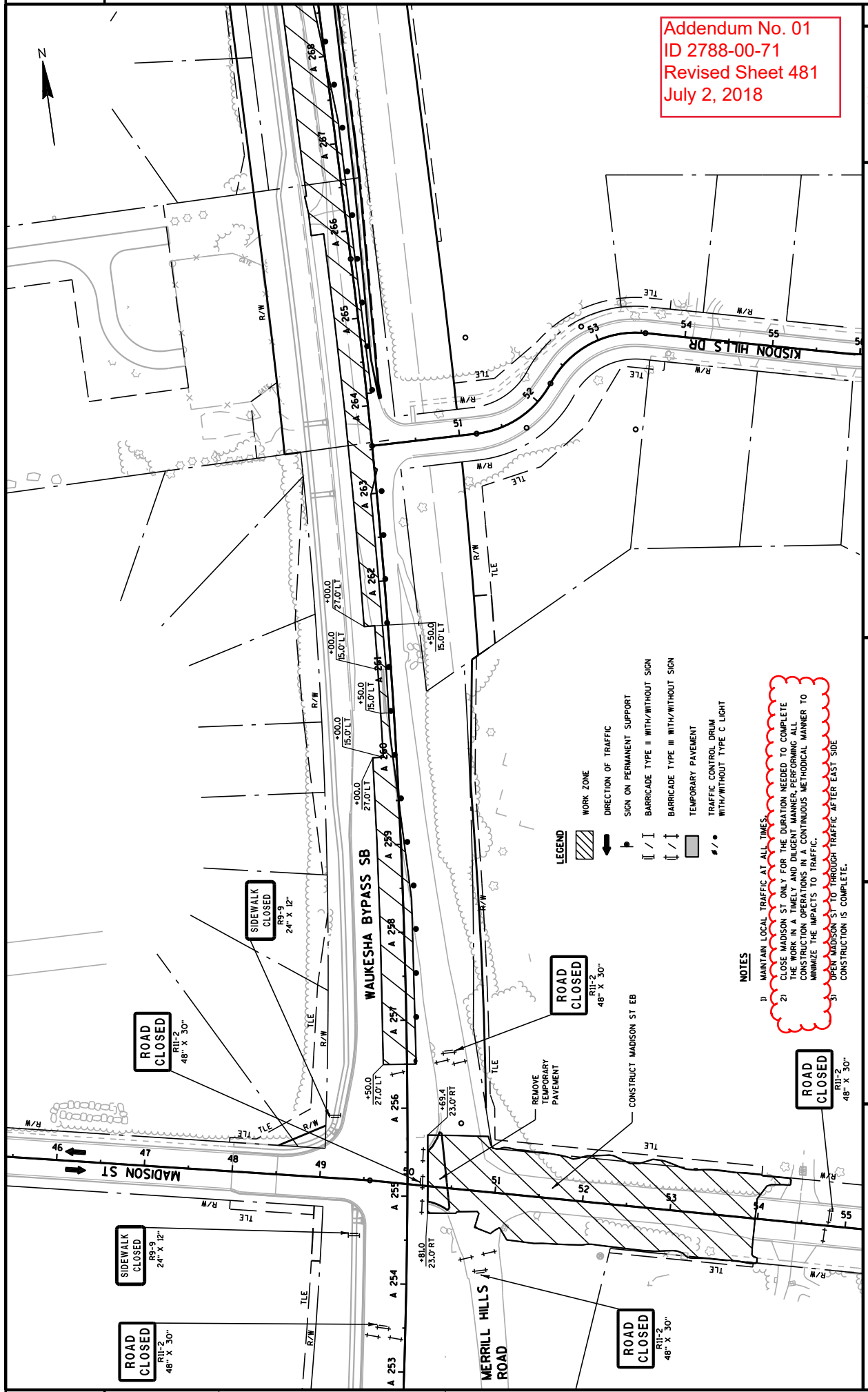


**Addendum No. 01**  
**ID 2788-00-71**  
**Revised Sheet 457**  
**July 2, 2018**

- LEGEND**
- WORK ZONE
  - DIRECTION OF TRAFFIC
  - SIGN ON PERMANENT SUPPORT
  - SIGN ON TEMPORARY SUPPORT
  - BARRIAGE TYPE III WITH/WITHOUT SIGN
  - TRAFFIC CONTROL DRUM
  - TRAFFIC CONTROL FLEXIBLE TUBULAR MARKER POSTS AND BASES
  - TEMPORARY PAVEMENT MARKING DASHED WHITE 4-INCH (6 FOOT LINE, 9 FOOT SKIP)
  - TEMPORARY PAVEMENT MARKING STOP LINE 18-INCH
  - TEMPORARY PAVEMENT MARKING WHITE 4-INCH
  - TEMPORARY PAVEMENT MARKING WHITE 8-INCH
  - TEMPORARY PAVEMENT MARKING DOUBLE YELLOW 4-INCH
  - TEMPORARY PAVEMENT MARKING REMOVABLE TAPE
  - TPMRT
  - TPMP



Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 481  
 July 2, 2018



- LEGEND**
- WORK ZONE
  - DIRECTION OF TRAFFIC
  - SIGN ON PERMANENT SUPPORT
  - BARRICADE TYPE II WITH/WITHOUT SIGN
  - BARRICADE TYPE III WITH/WITHOUT SIGN
  - TEMPORARY PAVEMENT
  - TRAFFIC CONTROL DRUM WITH/WITHOUT TYPE C LIGHT

- NOTES**
- 1) MAINTAIN LOCAL TRAFFIC AT ALL TIMES.
  - 2) CLOSE MADISON ST ONLY FOR THE DURATION NEEDED TO COMPLETE THE WORK IN A TIMELY AND DILIGENT MANNER, PERFORMING ALL CONSTRUCTION OPERATIONS IN A CONTINUOUS METHODOLOGICAL MANNER TO MINIMIZE THE IMPACTS TO TRAFFIC.
  - 3) OPEN MADISON ST TO THROUGH TRAFFIC AFTER EAST SIDE CONSTRUCTION IS COMPLETE.

| REMOVING PAVEMENT |                 |               |        |    |         |          |
|-------------------|-----------------|---------------|--------|----|---------|----------|
| CATEGORY          | ROADWAY         | LOCATION      | OFFSET | SY | REMARKS |          |
| 0010              | WAUKESHA BYPASS | A 270+37      | 17     | RT | 189     | DRIVEWAY |
|                   | SAYLESVILLE RD  | 16+00 - 18+00 | RT     |    | 3,370   |          |
|                   | GENESEE RD      | 45+00 - 50+00 | RT     |    | 14,691  |          |
| TOTAL             |                 |               |        |    | 18,250  |          |

| REMOVING CURB |          |               |        |    |         |
|---------------|----------|---------------|--------|----|---------|
| CATEGORY      | ROADWAY  | LOCATION      | OFFSET | LF | REMARKS |
| 0010          | KAMELTER | 47+27 - 49+18 | LT/RT  |    | 385     |
| TOTAL         |          |               |        |    | 385     |

| REMOVING ASPHALTIC SURFACE |                               |                     |        |       |                           |
|----------------------------|-------------------------------|---------------------|--------|-------|---------------------------|
| CATEGORY                   | ROADWAY                       | LOCATION            | OFFSET | SY    | REMARKS                   |
| 0010                       | SAYLESVILLE RD                | 13+00 - 14+50       | LT/RT  | 805   | OLD ALIGNMENT             |
|                            |                               | 14+50 - 16+50       | RT     | 700   | OLD ALIGNMENT             |
|                            | TEMP WAUKESHA BYPASS WIDENING | 93+50 - 99+67       | LT/RT  | 1,560 | CROSSOVER REMOVAL         |
|                            |                               | 149+00 - 153+90     | LT/RT  | 907   | CROSSOVER REMOVAL         |
|                            |                               | A 264+07 - A 269+97 | RT     | 89    | MERRILL HILLS RD WIDENING |
|                            |                               | A 270+48 - A 271+11 | RT     | 6     | MERRILL HILLS RD WIDENING |
|                            |                               | A 276+35 - A 277+85 | LT/RT  | 167   | CROSSOVER REMOVAL         |
|                            | TEMP GENESEE RD WIDENING      | 34+11 - 37+06       | RT     | 72    | WIDENING                  |
|                            |                               | 46+01 - 46+81       | RT     | 17    | WIDENING                  |
|                            |                               | 49+56 - 51+15       | RT     | 28    | WIDENING                  |
|                            |                               | 63+43 - 70+17       | LT/RT  | 1,600 | CROSSOVER REMOVAL         |
| TOTAL                      |                               |                     |        |       | 5,940                     |

| REMOVING CURB & GUTTER |                                |                     |        |       |           |
|------------------------|--------------------------------|---------------------|--------|-------|-----------|
| CATEGORY               | ROADWAY                        | LOCATION            | OFFSET | LF    | REMARKS   |
| 0010                   | WAUKESHA BYPASS                | A 254+82 - A 255+04 | LT     | 205   |           |
|                        |                                | A 254+83 - A 254+85 | RT     | 22    |           |
|                        |                                | A 255+20 - A 255+40 | RT     | 79    |           |
|                        |                                | A 255+46 - A 255+95 | LT/RT  | 225   |           |
|                        |                                | A 263+39 - A 263+39 | RT     | 51    |           |
|                        |                                | A 263+73 - A 263+95 | RT     | 89    |           |
|                        | SAYLESVILLE RD                 | 9+84 - 10+92        | LT     | 330   |           |
|                        |                                | 14+80 - 16+37       | RT     | 1,305 |           |
|                        | GENESEE RD                     | 50+28 - 53+73       | RT     | 1,763 |           |
|                        | MADISON ST                     | 48+00 - 48+51       | LT     | 51    |           |
|                        |                                | 48+00 - 48+51       | RT     | 51    |           |
|                        |                                | 51+62 - 53+97       | LT     | 235   |           |
|                        |                                | 51+62 - 53+97       | RT     | 235   |           |
|                        | KESDON HILLDR                  | 50+96 - 53+75       | LT     | 282   |           |
|                        |                                | 50+96 - 55+92       | RT     | 496   |           |
|                        | WEENERGIES DRIVEWAY - NORTH    | 10+17 - 10+17       | LT/RT  | 28    |           |
|                        | TEMP WAUKESHA BYPASS CROSSOVER | 93+50 - 99+67       | LT/RT  | 1,230 | CROSSOVER |
|                        |                                | A 276+35 - A 277+85 | LT/RT  | 300   | CROSSOVER |
|                        | TEMP GENESEE RD CROSSOVER      | 63+43 - 70+17       | LT/RT  | 1,443 | CROSSOVER |
| TOTAL                  |                                |                     |        |       | 8,440     |

Addendum No. 01  
ID 2788-00-71  
Revised Sheet 554  
July 2, 2018

Addendum No. 01  
ID 2788-00-71  
Revised Sheet 557  
July 2, 2018

| CATEGORY | DIVISION            | FROM/TO STATION                                                                                                                                                                                                                      | LOCATION                                                                                                                                                                                                                                                                                                      | EXCAVATION COMMON (CY)                                                                                |                                 | EXCAVATION ROCK (CY)  | EXCAVATION MARSH (CY)  | STRUCTURE EXCAVATION (CY) | FILL (CY) | ROADWAY EMBANKMENT (CY) | MASS ORDINATE +/- (5) |   |
|----------|---------------------|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-------------------------------------------------------------------------------------------------------|---------------------------------|-----------------------|------------------------|---------------------------|-----------|-------------------------|-----------------------|---|
|          |                     |                                                                                                                                                                                                                                      |                                                                                                                                                                                                                                                                                                               | (1)                                                                                                   | (2)                             |                       |                        |                           |           |                         |                       |   |
| 0010     | 1                   | 100+37 - 273+75<br>33+75 - 38+25                                                                                                                                                                                                     | Waukesha Bypass Structures<br>Genesee Road Temp Widen                                                                                                                                                                                                                                                         | 0                                                                                                     | 81                              | 0                     | 0                      | 0                         | 0         | 0                       | 0                     |   |
|          | DIVISION 1 SUBTOTAL |                                                                                                                                                                                                                                      |                                                                                                                                                                                                                                                                                                               |                                                                                                       |                                 |                       |                        |                           |           |                         |                       |   |
| 0010     | 2                   | 100+37 - 173+00<br>173+00 - 273+75<br>34+11 - 61+84<br>201+00<br>214+00<br>50+97 - 56+30<br>48+54 - 49+44<br>48+00 - 49+25<br>10+64 - 12+88<br>300+68 - 301+75<br>10+68 - 12+77<br>10+50 - 12+42<br>10+30 - 12+68<br>110+10 - 111+94 | Waukesha Bypass<br>Waukesha Bypass<br>Genesee Road<br>By Pass - Bioretention Ponds<br>By Pass - Bioretention Ponds<br>MacArthur Road<br>Merrill Hills Court<br>Madison Street<br>Driveway Genesee Road 47+25<br>Driveway 208+25<br>Driveway 222+76<br>Driveway 240+25<br>Driveway W North<br>Driveway W South | 66,434<br>99,141<br>16,345<br>718<br>2,203<br>40<br>284<br>402<br>515<br>297<br>20<br>850<br>38<br>45 | 205,0100<br>EBS EXCAVATION (CY) | 205,0200<br>ROCK (CY) | 205,0400<br>MARSH (CY) | 0                         | 0         | 0                       | 0                     | 0 |
|          | DIVISION 2 SUBTOTAL |                                                                                                                                                                                                                                      |                                                                                                                                                                                                                                                                                                               |                                                                                                       |                                 |                       |                        |                           |           |                         |                       |   |
| 0010     | 3                   | 153+00 - 155+50<br>43+00 - 64+00                                                                                                                                                                                                     | Waukesha Bypass<br>Sunset Drive                                                                                                                                                                                                                                                                               | 4,043<br>1,828                                                                                        | 2,174<br>4,257                  | 0                     | 0                      | 0                         | 0         | 0                       | 0                     |   |
|          | DIVISION 3 SUBTOTAL |                                                                                                                                                                                                                                      |                                                                                                                                                                                                                                                                                                               |                                                                                                       |                                 |                       |                        |                           |           |                         |                       |   |
| 0010     | 4                   | 111+00 - 112+60<br>50+78 - 52+00                                                                                                                                                                                                     | Waukesha Bypass<br>Genesee Road                                                                                                                                                                                                                                                                               | 2,090<br>839                                                                                          | 0<br>0                          | 0                     | 0                      | 0                         | 0         | 0                       | 0                     |   |
|          | DIVISION 4 SUBTOTAL |                                                                                                                                                                                                                                      |                                                                                                                                                                                                                                                                                                               |                                                                                                       |                                 |                       |                        |                           |           |                         |                       |   |
| 0010     | 5                   | 100+37 - 113+00<br>34+11 - 49+44<br>7+50 - 19+37                                                                                                                                                                                     | Waukesha Bypass<br>Genesee Road<br>Sawlesville Road                                                                                                                                                                                                                                                           | 3,581<br>10,847<br>10,573                                                                             | 0<br>763<br>810                 | 0                     | 0                      | 0                         | 0         | 0                       | 0                     |   |
|          | DIVISION 5 SUBTOTAL |                                                                                                                                                                                                                                      |                                                                                                                                                                                                                                                                                                               |                                                                                                       |                                 |                       |                        |                           |           |                         |                       |   |
| 0010     | 6                   | 174+00 - 176+50<br>219+00 - 224+00<br>240+00 - 244+00<br>255+00 - 255+50<br>256+50 - 260+50<br>261+00 - 271+00<br>48+05 - 49+12<br>50+55 - 53+97<br>48+25 - 52+50                                                                    | Waukesha Bypass<br>Waukesha Bypass<br>Waukesha Bypass<br>Waukesha Bypass<br>Waukesha Bypass<br>Kame Terrace<br>Madison Street<br>Glacial Drumlins Trail                                                                                                                                                       | 107<br>3,864<br>972<br>104<br>4,341<br>706<br>336<br>216<br>1,099                                     | 205,0100<br>EBS EXCAVATION (CY) | 205,0200<br>ROCK (CY) | 205,0400<br>MARSH (CY) | 0                         | 0         | 0                       | 0                     |   |
|          | DIVISION 6 SUBTOTAL |                                                                                                                                                                                                                                      |                                                                                                                                                                                                                                                                                                               |                                                                                                       |                                 |                       |                        |                           |           |                         |                       |   |
|          | GRAND TOTAL         |                                                                                                                                                                                                                                      |                                                                                                                                                                                                                                                                                                               |                                                                                                       |                                 |                       |                        |                           |           |                         |                       |   |

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ID 2788-00-71  
Revised Sheet 558  
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| CATEGORY | DIVISION | FROM/TO STATION | LOCATION                           | EXCAVATION COMMON (CY) (1) |                         | EXCAVATION MARSH (CY) (7) | STRUCTURE EXCAVATION (CY) (6) | FILL (CY) | ROADWAY EMBANKMENT (CY) (4) | MASS ORDINATE +/- (5) |
|----------|----------|-----------------|------------------------------------|----------------------------|-------------------------|---------------------------|-------------------------------|-----------|-----------------------------|-----------------------|
|          |          |                 |                                    | CUT (CY) (2)               | EBS EXCAVATION (CY) (3) |                           |                               |           |                             |                       |
| 0010     | 7        | 173+00 - 273+75 | WAUKESHA BYPASS                    | 52,285                     | 4,649                   | 0                         | 0                             | 34,623    | 34,623                      | 17,662                |
|          |          | 49+00 - 61+84   | GENESSEE ROAD                      | 4,332                      | 184                     | 0                         | 0                             | 1,460     | 1,460                       | 2,872                 |
|          |          | 61+00 - 62+30   | MERRILL HILLS ROAD                 | 264                        | 0                       | 0                         | 0                             | 230       | 230                         | 34                    |
|          |          | 71+05 - 72+33   | MERRILL HILLS ROAD                 | 254                        | 0                       | 0                         | 0                             | 71        | 71                          | 182                   |
|          |          | 56+30 - 57+60   | MACARTHUR ROAD                     | 431                        | 0                       | 0                         | 0                             | 3         | 3                           | 427                   |
|          |          | 75+50 - 78+75   | MERRILL HILLS ROAD AT MACARTHUR    | 358                        | 0                       | 0                         | 0                             | 1         | 1                           | 357                   |
|          |          | 82+32 - 83+63   | MERRILL HILLS ROAD                 | 158                        | 0                       | 0                         | 0                             | 1,597     | 1,597                       | -1,440                |
|          |          | 89+88 - 91+19   | MERRILL HILLS ROAD                 | 202                        | 0                       | 0                         | 0                             | 349       | 349                         | -147                  |
|          |          | 50+00 - 54+50   | KISDON HILL DRIVE TEMP WIDEN (3-1) | 1,159                      | 0                       | 0                         | 0                             | 0         | 0                           | 1,159                 |
|          |          | 50+95 - 53+75   | KISDON HILL DRIVE (3-2)            | 1,622                      | 0                       | 0                         | 0                             | 0         | 0                           | 1,636                 |
|          |          | 50+95 - 53+75   | KISDON HILL DRIVE (3-3)            | 67                         | 0                       | 13                        | 0                             | 398       | 398                         | -317                  |
|          |          | 45+80 - 54+82   | GLACIAL DRUMLIN TRAIL              | 649                        | 0                       | 0                         | 0                             | 34        | 34                          | 616                   |
|          |          | 10+05 - 13+85   | GLACIAL DRUMLIN TRAIL SW           | 1,455                      | 0                       | 0                         | 0                             | 291       | 291                         | 1,164                 |
|          |          | 17+64 - 19+90   | GLACIAL DRUMLIN TRAIL SE           | 36                         | 0                       | 0                         | 0                             | 1,012     | 1,012                       | -975                  |
|          |          | 30+00 - 31+64   | GLACIAL DRUMLIN TRAIL N            | 13                         | 0                       | 0                         | 0                             | 578       | 578                         | -564                  |
|          |          | 70+50 - 71+41   | GLACIAL DRUMLIN TRAIL AT GRADE     | 21                         | 0                       | 0                         | 0                             | 36        | 36                          | -15                   |
|          |          | 50+68 - 51+78   | DRIVEWAY 270+01                    | 12                         | 0                       | 0                         | 0                             | 259       | 259                         | -247                  |
|          |          | 50+68 - 51+72   | DRIVEWAY 270+36                    | 14                         | 0                       | 0                         | 0                             | 178       | 178                         | -163                  |
|          |          |                 |                                    | 63,332                     | 4,834                   | 27                        | 0                             | 41,119    | 41,119                      | 22,240                |
|          |          |                 |                                    | 339                        | 0                       | 0                         | 0                             | 11        | 11                          | 328                   |
|          |          |                 |                                    | 339                        | 0                       | 0                         | 0                             | 11        | 11                          | 328                   |
|          |          |                 |                                    | 0                          | 4,486                   | 2,841                     | 0                             | 0         | 0                           | 0                     |
|          |          |                 |                                    | 326,629                    | 113,849                 | 59,668                    | 0                             | 465,207   | 465,207                     | -138,467              |
|          |          |                 |                                    | 440,477                    | 440,477                 | 0                         | 0                             | 0         | 0                           | 0                     |
|          |          |                 |                                    | TOTAL COMMON EXC           |                         |                           |                               |           |                             |                       |

- EXCAVATION COMMON = CUT + EBS EXCAVATION. ITEM NUMBER 205.0100.
- CUT VOLUME INCLUDES CONCRETE AND ASPHALTIC SURFACE MATERIAL.
- EBS EXCAVATION TO BE BACKFILLED WITH SELECT CRUSHED MATERIAL UNLESS OTHERWISE NOTED IN PLANS. EXCAVATED MATERIAL IS USABLE IN FILLS OUTSIDE THE 1:1 SLOPE IN EMBANKMENTS WITH HEIGHT BELOW 10 FEET.
- ROADWAY EMBANKMENT = FILL
- MASS ORDINATE IS CALCULATED BY DIVISION. A POSITIVE QUANTITY INDICATES AN EXCESS OF MATERIAL WITHIN THE DIVISION AND A NEGATIVE QUANTITY INDICATES A SHORTAGE OF MATERIAL WITHIN THE DIVISION.
- MASS ORDINATE = CUT + EXCAVATION ROCK - FILL. THE MASS ORDINATE DOES NOT INCLUDE EBS AND MARSH EXCAVATION, WHICH ARE USABLE AS NOTED ON THIS SHEET AND SPECIAL PROVISIONS. THE MASS ORDINATE IS FOR INFORMATION PURPOSES ONLY AS COMMON EXCAVATION AND ROADWAY EMBANKMENT ARE NOT BALANCED FOR QUANTITY PURPOSES AND DOES NOT GUARANTEE THE QUALITY OF COMMON EXCAVATION, AND IF IT CAN BE REUSED ON SITE.
- STRUCTURE EXCAVATION LIMITS FOR RETAINING WALL CONSTRUCTION ARE SHOWN IN THE CROSS SECTIONS AND ARE ASSUMED TO BE 70% OF THE RETAINING WALL HEIGHT. THIS IS FOR INFORMATIONAL PURPOSES ONLY, AND WILL VARY DEPENDING ON SHOP DRAWING DESIGN AND WILL VARY DEPENDING ON SHOP DRAWING DESIGN.
- EXCAVATION MARSH LIMITS AS IDENTIFIED IN THE CROSS SECTIONS. MARSH EXCAVATION WILL NOT BE ALLOWED WITHIN EMBANKMENTS. DISPOSE OF AS DESCRIBED IN SECTION 205.3. SEE MARSH EXCAVATION AND TRENCH DETAIL FOR FILL REQUIREMENTS. MARSH EXCAVATION TO BE BACKFILLED WITH BREAKER RUN UNLESS OTHERWISE NOTED IN PLANS.
- EXCAVATED PETROLEUM CONTAMINATED MATERIAL TO BE PAID UNDER ITEM 205.0501.S. EXCAVATION LOADING HAULING AND DISPOSAL OF PETROLEUM CONTAMINATED SOLD BY THE TON.





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Revised Sheet 570  
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ASPHALTIC PAVEMENT ITEMS (CONTINUED)

| 440.4410                           | 450.4000                | 460.2000            | 460.6223      | 460.6224 | 465.0120                    | 465.0125          | REMARKS                      |
|------------------------------------|-------------------------|---------------------|---------------|----------|-----------------------------|-------------------|------------------------------|
| INCENTIVE WEATHER IRI RIDE         | HMA COLD WEATHER PAVING | INCENTIVE TACK COAT | PAVEMENT TYPE | HMA TYPE | ASPHALTIC DRIVEWAYS SURFACE | ASPHALTIC SURFACE |                              |
| DOL                                | TON                     | DOL                 | TON           | TON      | TON                         | TON               |                              |
| 33+90 - 38+15                      | --                      | --                  | --            | --       | 122                         | 122               | STAGE 1 (EB)                 |
| 46+01 - 46+81                      | --                      | --                  | --            | --       | 26                          | 26                | STAGE 1 (EB)                 |
| 49+56 - 51+15                      | --                      | --                  | --            | --       | 82                          | 82                | STAGE 1 (NB)                 |
| 34+11 - 37+06                      | --                      | --                  | --            | --       | 47                          | 47                | STAGE 2A (WB)                |
| 62+43 - 70+17                      | --                      | --                  | --            | --       | 440                         | 440               | STAGE 1; TEMPORARY CROSSOVER |
| <b>TEMP KAME TERR WIDENING</b>     |                         |                     |               |          |                             |                   |                              |
| 45+60 - 48+05                      | --                      | --                  | --            | --       | 9                           | 9                 | STA GE 2A                    |
| <b>TEMP MADISON ST WIDENING</b>    |                         |                     |               |          |                             |                   |                              |
| 50+20 - 50+50                      | --                      | --                  | --            | --       | 45                          | 45                | STAGE 2E                     |
| <b>TEMP KISON HILL DR WIDENING</b> |                         |                     |               |          |                             |                   |                              |
| 50+00 - 53+75                      | --                      | --                  | --            | --       | 314                         | 314               | STAGE 3                      |
| <b>TOTALS</b>                      | 6,840                   | 600                 | 2,500         | 11,680   | 9,150                       | 8,900             | 2,400                        |
|                                    |                         |                     |               |          | 630                         |                   | 1,900                        |

\* ADDITIONAL QUANTITIES SHOWN ELSEWHERE

**SURFACE DRAIN ITEMS**

| 416.1010               | 465.0315        | REMARKS |
|------------------------|-----------------|---------|
| CONCRETE SURFACE DRAIN | ASPHALTIC FLUME |         |
| CY                     | SY              |         |
| 10+56                  | 25.2 RT         | --      |
| 47+82                  | 42.4 RT         | 12.9    |
| 49+11                  | 60.8 RT         | --      |
| 51+53                  | 37.4 RT         | 30.2    |
| 55+96                  | 42.4 RT         | --      |
| 56+45                  | 37.4 RT         | 0.4     |
| 56+75                  | 56.2 RT         | 0.3     |
| 56+77                  | 68.2 LT         | 0.4     |
| 57+14                  | 16.7 RT         | 0.3     |
| 57+23                  | 14.6 LT         | 0.3     |
| <b>TOTAL</b>           |                 | 23.5    |

| 0010         | WALKESHA BYPASS | STATION  | OFFSET   | SY   | REMARKS    |
|--------------|-----------------|----------|----------|------|------------|
|              |                 | 114+16   | 84.9 LT  | 1.5  | --         |
|              |                 | 114+19   | 106.3 RT | 3.9  | --         |
|              |                 | 127+39   | 40.0 RT  | 1.3  | DROP INLET |
|              |                 | 130+54   | 40.0 RT  | 0.8  | DROP INLET |
|              |                 | A 141+95 | 40.0 LT  | 0.9  | --         |
|              |                 | A 149+37 | 64.8 RT  | 1.7  | --         |
|              |                 | A 155+41 | 56.8 RT  | 0.9  | --         |
|              |                 | A 155+64 | 81.5 LT  | 0.7  | --         |
|              |                 | A 172+43 | 53.7 RT  | 1.0  | --         |
|              |                 | A 205+03 | 47.0 RT  | 0.7  | --         |
|              |                 | A 216+03 | 47.0 RT  | 0.6  | --         |
|              |                 | A 225+41 | 46.9 LT  | 0.9  | --         |
|              |                 | A 225+97 | 47.0 RT  | 0.6  | --         |
|              |                 | A 233+73 | 48.0 LT  | 0.7  | --         |
|              |                 | A 237+97 | 52.5 LT  | 0.8  | --         |
|              |                 | A 254+18 | 77.5 RT  | 0.6  | --         |
|              |                 | A 270+31 | 47.0 LT  | 1.2  | --         |
|              |                 | 34+40    | 28.8 LT  | 15.1 | --         |
|              |                 | 44+66    | 82.7 RT  | 10.3 | --         |
|              |                 | 51+72    | 76.3 RT  | 25.6 | --         |
|              |                 | 56+02    | 53.6 LT  | 11.3 | --         |
|              |                 | 56+07    | 46.0 RT  | --   | DROP INLET |
| <b>TOTAL</b> |                 |          |          | 6.6  |            |

**COLD PATCH**

| 0010         | PROJECT | TON |
|--------------|---------|-----|
|              |         | 20  |
| <b>TOTAL</b> |         | 20  |

**STORM SEWER ROCK EXCAVATION**

| 0100         | STAGE         | ROADWAY         | LOCATION        | OFFSET | CY  |
|--------------|---------------|-----------------|-----------------|--------|-----|
|              | 2E3           | WALKESHA BYPASS | 259+00 - 266+00 | RT     | 60  |
| 3            | KISON HILL DR |                 | 51+00 - 52+00   | LT/RT  | 60  |
| <b>TOTAL</b> |               |                 |                 |        | 120 |

ALL ITEMS CATEGORY 0100 UNLESS OTHERWISE NOTED

| STRUCTURE NUMBER            | STATION | OFFSET  | CENTER OF STRUCTURE | LOWEST INVERT | DEPTH | 522,1042*<br>APRON ENDWALLS FOR CULVERT PIPE REINFORCED CONCRETE 42-INCH EACH | 522,2614*<br>APRON ENDWALLS FOR CULVERT PIPE REINFORCED CONCRETE HORIZONTAL ELLIPTICAL 14x23-INCH EACH | 522,2619*<br>APRON ENDWALLS FOR CULVERT PIPE REINFORCED CONCRETE HORIZONTAL ELLIPTICAL 19x30-INCH EACH | 522,2624*<br>APRON ENDWALLS FOR CULVERT PIPE REINFORCED CONCRETE HORIZONTAL ELLIPTICAL 24x38-INCH EACH | 522,2629*<br>APRON ENDWALLS FOR CULVERT PIPE REINFORCED CONCRETE HORIZONTAL ELLIPTICAL 28x45-INCH EACH | 611,0530<br>MANHOLE COVERS TYPE J | 611,0535<br>MANHOLE COVERS TYPE J-SPECIAL | 611,0624<br>INLET COVERS TYPE H |    |   |   |   |    |    |
|-----------------------------|---------|---------|---------------------|---------------|-------|-------------------------------------------------------------------------------|--------------------------------------------------------------------------------------------------------|--------------------------------------------------------------------------------------------------------|--------------------------------------------------------------------------------------------------------|--------------------------------------------------------------------------------------------------------|-----------------------------------|-------------------------------------------|---------------------------------|----|---|---|---|----|----|
| 750                         | 51+76   | 46.8 RT | 855.00              | 848.28        | 5.47  | --                                                                            | --                                                                                                     | --                                                                                                     | --                                                                                                     | --                                                                                                     | 1                                 | --                                        | --                              |    |   |   |   |    |    |
| 750.1                       | 52+84   | 35.0 LT | 860.61              | 856.71        | 2.90  | --                                                                            | --                                                                                                     | --                                                                                                     | --                                                                                                     | --                                                                                                     | 1                                 | --                                        | 1                               |    |   |   |   |    |    |
| 750.10                      | 50+51   | 61.0 LT | 856.80              | 851.27        | 4.53  | --                                                                            | --                                                                                                     | --                                                                                                     | --                                                                                                     | --                                                                                                     | --                                | --                                        | --                              |    |   |   |   |    |    |
| 750.11                      | 50+73   | 72.3 RT | 853.17              | 846.75        | 5.42  | --                                                                            | --                                                                                                     | --                                                                                                     | --                                                                                                     | --                                                                                                     | --                                | --                                        | --                              |    |   |   |   |    |    |
| 750.13                      | 50+23   | 53.8 RT | 852.67              | 845.75        | 5.92  | --                                                                            | --                                                                                                     | --                                                                                                     | --                                                                                                     | --                                                                                                     | --                                | --                                        | --                              |    |   |   |   |    |    |
| 750.14                      | 52+00   | 46.8 RT | --                  | 843.69        | --    | --                                                                            | --                                                                                                     | --                                                                                                     | --                                                                                                     | --                                                                                                     | 1                                 | --                                        | --                              |    |   |   |   |    |    |
| 750.15                      | 53+65   | 35.0 RT | --                  | 861.90        | --    | --                                                                            | --                                                                                                     | --                                                                                                     | --                                                                                                     | --                                                                                                     | --                                | --                                        | --                              |    |   |   |   |    |    |
| 750.2                       | 52+88   | 25.0 RT | 861.15              | 857.00        | 3.15  | --                                                                            | --                                                                                                     | --                                                                                                     | --                                                                                                     | --                                                                                                     | --                                | --                                        | 1                               |    |   |   |   |    |    |
| 750.3                       | 52+88   | 46.4 RT | 861.00              | 856.66        | 3.09  | --                                                                            | --                                                                                                     | --                                                                                                     | --                                                                                                     | --                                                                                                     | 1                                 | --                                        | --                              |    |   |   |   |    |    |
| 750.4                       | 51+76   | 25.0 RT | 856.71              | 852.95        | 2.76  | --                                                                            | --                                                                                                     | --                                                                                                     | --                                                                                                     | --                                                                                                     | --                                | --                                        | --                              |    |   |   |   |    |    |
| 750.5                       | 51+00   | 35.0 LT | 856.24              | 852.30        | 2.94  | --                                                                            | --                                                                                                     | --                                                                                                     | --                                                                                                     | --                                                                                                     | --                                | --                                        | --                              |    |   |   |   |    |    |
| 750.6                       | 51+76   | 35.0 LT | 856.93              | 853.30        | 2.63  | --                                                                            | --                                                                                                     | --                                                                                                     | --                                                                                                     | --                                                                                                     | --                                | --                                        | 1                               |    |   |   |   |    |    |
| 750.7                       | 52+19   | 35.0 LT | 858.10              | 854.06        | 3.04  | --                                                                            | --                                                                                                     | --                                                                                                     | --                                                                                                     | --                                                                                                     | --                                | --                                        | 1                               |    |   |   |   |    |    |
| 750.9                       | 50+81   | 62.0 LT | 855.92              | 851.56        | 4.36  | --                                                                            | --                                                                                                     | --                                                                                                     | --                                                                                                     | --                                                                                                     | 1                                 | --                                        | 4                               |    |   |   |   |    |    |
| <b>MADISON ST SUBTOTALS</b> |         |         |                     |               |       |                                                                               |                                                                                                        |                                                                                                        |                                                                                                        |                                                                                                        |                                   |                                           | 1                               | 15 | 4 |   |   |    |    |
| <b>PROJECT TOTALS*</b>      |         |         |                     |               |       |                                                                               |                                                                                                        |                                                                                                        |                                                                                                        |                                                                                                        |                                   |                                           | 2                               | 6  | 1 | 1 | 1 | 15 | 26 |

\*ADDITIONAL QUANTITIES FOUND ELSEWHERE

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ID 2788-00-71  
Revised Sheet 599  
July 2, 2018

ALL ITEMS CATEGORY 0100 UNLESS OTHERWISE NOTED

STORM SEWER ITEMS (CONTINUED)

| STRUCTURE NUMBER            | STATION | OFFSET | CENTER OF STRUCTURE | LOWEST INVERT | DEPTH | INLET COVERS TYPE HM-GJ EACH | INLET COVERS TYPE H-S EACH | INLET COVERS TYPE MS EACH | INLET COVERS TYPE V EACH | CATCH BASINS 4-FT DIAMETER EACH | CATCH BASINS 5-FT DIAMETER EACH | CATCH BASINS 6-FT DIAMETER EACH | CATCH BASINS 2X3-FT DIAMETER EACH | MANHOLES 4-FT DIAMETER EACH | MANHOLES 5-FT DIAMETER EACH | MANHOLES 6-FT DIAMETER EACH | STORM SEWER ITEMS (CONTINUED) |          |          |          |          |          |          |          |          |          |          |          |    |    |    |   |
|-----------------------------|---------|--------|---------------------|---------------|-------|------------------------------|----------------------------|---------------------------|--------------------------|---------------------------------|---------------------------------|---------------------------------|-----------------------------------|-----------------------------|-----------------------------|-----------------------------|-------------------------------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----|----|----|---|
|                             |         |        |                     |               |       |                              |                            |                           |                          |                                 |                                 |                                 |                                   |                             |                             |                             | 611.0630                      | 611.0633 | 611.0638 | 611.0642 | 611.0654 | 611.1004 | 611.1005 | 611.1006 | 611.1230 | 611.2004 | 611.2005 | 611.2006 |    |    |    |   |
| 750                         | 51+76   | 46.8   | RT 855.00           | 848.28        | 5.47  | --                           | --                         | --                        | --                       | --                              | --                              | --                              | --                                | 1                           | --                          | --                          | --                            | 1        | 1        |          |          |          |          |          |          |          |          |          |    |    |    |   |
| 750.1                       | 52+84   | 35.0   | LT 860.61           | 856.71        | 2.90  | --                           | --                         | --                        | --                       | --                              | --                              | --                              | --                                | --                          | --                          | --                          | --                            | --       | --       |          |          |          |          |          |          |          |          |          |    |    |    |   |
| 750.10                      | 50+51   | 61.0   | LT 856.80           | 851.27        | 4.53  | 1                            | --                         | --                        | --                       | --                              | --                              | --                              | --                                | --                          | --                          | --                          | --                            | --       | --       |          |          |          |          |          |          |          |          |          |    |    |    |   |
| 750.11                      | 50+73   | 72.3   | RT 853.17           | 846.75        | 5.42  | 1                            | --                         | --                        | --                       | --                              | --                              | --                              | --                                | --                          | --                          | --                          | --                            | --       | --       |          |          |          |          |          |          |          |          |          |    |    |    |   |
| 750.13                      | 50+23   | 53.8   | RT 852.67           | 845.75        | 5.92  | 1                            | --                         | --                        | --                       | --                              | --                              | --                              | --                                | --                          | --                          | --                          | --                            | --       | --       |          |          |          |          |          |          |          |          |          |    |    |    |   |
| 750.14                      | 52+00   | 46.8   | RT 843.89           | --            | --    | --                           | --                         | --                        | --                       | --                              | --                              | --                              | --                                | --                          | --                          | --                          | --                            | --       | --       |          |          |          |          |          |          |          |          |          |    |    |    |   |
| 750.15                      | 53+65   | 35.0   | RT 861.90           | 861.90        | --    | --                           | --                         | --                        | --                       | --                              | --                              | --                              | --                                | --                          | --                          | --                          | --                            | --       | --       |          |          |          |          |          |          |          |          |          |    |    |    |   |
| 750.2                       | 52+88   | 25.0   | RT 861.15           | 861.15        | 3.15  | --                           | --                         | --                        | --                       | --                              | --                              | --                              | --                                | --                          | --                          | --                          | --                            | --       | --       |          |          |          |          |          |          |          |          |          |    |    |    |   |
| 750.3                       | 52+88   | 46.4   | RT 861.00           | 856.66        | 3.09  | --                           | --                         | --                        | --                       | --                              | --                              | --                              | --                                | 1                           | --                          | --                          | --                            | --       | --       |          |          |          |          |          |          |          |          |          |    |    |    |   |
| 750.4                       | 51+76   | 25.0   | RT 856.71           | 852.95        | 2.76  | 1                            | --                         | --                        | --                       | --                              | --                              | --                              | --                                | --                          | --                          | --                          | --                            | --       | --       |          |          |          |          |          |          |          |          |          |    |    |    |   |
| 750.5                       | 51+00   | 35.0   | LT 856.24           | 852.30        | 2.94  | 1                            | --                         | --                        | --                       | --                              | --                              | --                              | --                                | --                          | --                          | --                          | --                            | --       | --       |          |          |          |          |          |          |          |          |          |    |    |    |   |
| 750.6                       | 51+76   | 35.0   | LT 856.93           | 853.30        | 2.63  | --                           | --                         | --                        | --                       | --                              | --                              | --                              | --                                | --                          | --                          | --                          | --                            | --       | --       |          |          |          |          |          |          |          |          |          |    |    |    |   |
| 750.7                       | 52+19   | 35.0   | LT 858.10           | 854.06        | 3.04  | --                           | --                         | --                        | --                       | --                              | --                              | --                              | --                                | --                          | --                          | --                          | --                            | --       | --       |          |          |          |          |          |          |          |          |          |    |    |    |   |
| 750.9                       | 50+81   | 62.0   | LT 855.92           | 851.56        | 4.36  | --                           | --                         | 1                         | --                       | --                              | --                              | --                              | --                                | 1                           | --                          | --                          | --                            | --       | --       |          |          |          |          |          |          |          |          |          |    |    |    |   |
| <b>MADISON ST SUBTOTALS</b> |         |        |                     |               |       |                              |                            |                           |                          |                                 |                                 |                                 |                                   |                             |                             |                             | 10                            | 1        | 3        | 3        | 1        | 6        | 25       | 17       | 4        | 50       | 25       | 3        | 3  | 24 | 21 | 8 |
| <b>PROJECT TOTALS*</b>      |         |        |                     |               |       |                              |                            |                           |                          |                                 |                                 |                                 |                                   |                             |                             |                             | 204                           | 17       | 4        | 50       | 25       | 6        | 1        | 3        | 3        | 3        | 3        | 24       | 21 | 8  |    |   |

\*ADDITIONAL QUANTITIES FOUND ELSEWHERE

Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 610  
 July 2, 2018

Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 642  
 July 2, 2018

ADJUSTING STORM SEWER STRUCTURE

| CATEGORY | STAGE                  | ROADWAY                   | STRUCTURE NUMBER | STATION | OFFSET  | EXISTING |         | PROPOSED |         | 611.0610 | 611.19710                                              | REMARKS                                                 |
|----------|------------------------|---------------------------|------------------|---------|---------|----------|---------|----------|---------|----------|--------------------------------------------------------|---------------------------------------------------------|
|          |                        |                           |                  |         |         | RIM ELEV | FT      | RIM ELEV | FT      |          |                                                        |                                                         |
| 0010     | 1                      | GENESEE RD. CROSSOVER     | Ex-1             | 62+64   | 8.9 RT  | 798.47   | 798.47  | 798.47   | 798.47  | 1        | 1                                                      | SALVAGED INLET COVER TYPE WM PLACE INLET COVER TYPE BW  |
|          |                        |                           | Ex-1             | 62+64   | 8.9 RT  | --       | 798.47  | 798.47   | --      | 1        | 1                                                      | SALVAGE INLET COVER TYPE BW REPLACE INLET COVER TYPE WM |
|          | 1                      | LES PALIL PKWY. CROSSOVER | Ex-2             | 93+88   | RT      | +797.50  | +797.50 | +797.50  | +797.50 | 1        | 1                                                      | SALVAGED INLET COVER TYPE WM PLACE INLET COVER TYPE BW  |
|          |                        |                           | Ex-3             | 94+72   | LT      | +797.00  | +797.00 | +797.00  | +797.00 | 1        | 1                                                      | SALVAGED INLET COVER TYPE WM PLACE INLET COVER TYPE BW  |
| Ex-4     |                        |                           | 97+57            | LT      | +799.10 | +799.10  | +799.10 | +799.10  | 1       | 1        | SALVAGED INLET COVER TYPE WM PLACE INLET COVER TYPE BW |                                                         |
| 3        | STA 93+50 TO STA 99+68 | Ex-5                      | 97+58            | RT      | +799.30 | +799.30  | +799.30 | +799.30  | 1       | 1        | SALVAGED INLET COVER TYPE WM PLACE INLET COVER TYPE BW |                                                         |
|          |                        | Ex-2                      | 93+88            | RT      | --      | +797.50  | --      | +797.50  | --      | 1        | 1                                                      | SALVAGE INLET COVER TYPE BW REPLACE INLET COVER TYPE WM |
|          |                        | Ex-3                      | 94+72            | LT      | --      | +797.00  | --      | +797.00  | --      | 1        | 1                                                      | SALVAGE INLET COVER TYPE BW REPLACE INLET COVER TYPE WM |
| 3        | STA 93+50 TO STA 99+71 | Ex-4                      | 97+57            | LT      | --      | +799.10  | --      | +799.10  | --      | 1        | 1                                                      | SALVAGE INLET COVER TYPE BW REPLACE INLET COVER TYPE WM |
|          |                        | Ex-5                      | 97+58            | RT      | --      | +799.30  | --      | +799.30  | --      | 1        | 1                                                      | SALVAGE INLET COVER TYPE BW REPLACE INLET COVER TYPE WM |
| TOTALS   |                        |                           |                  |         |         |          |         |          |         | 5        | 10                                                     |                                                         |

REMARKS

TEMPORARY PRECAST TRENCH DRAIN

| CATEGORY | ROADWAY                    | STATION       | SPV_0090.08 |
|----------|----------------------------|---------------|-------------|
| 0010     | TEMP.GENESEE RD. CROSSOVER | 62+70 - 65+00 | 230         |
| TOTAL    |                            |               | 230         |

3

DRAIN TILE ITEMS

| CATEGORY | ROADWAY         | LOCATION            | OFFSET | LF | LF  | LF  |
|----------|-----------------|---------------------|--------|----|-----|-----|
| 0010     | WAUKESHA BYPASS | 612.0206            | PIPE   | 10 | 10  | 100 |
|          |                 | UNDERDRAIN          | 30     | 20 | 200 |     |
|          |                 | UNPERFORATED        | 10     | 10 | 100 |     |
|          |                 | 6-INCH UNPERFORATED | 50     | 40 | 400 |     |
| TOTALS   |                 |                     |        |    |     | 400 |

\* ADDITIONAL QUANTITIES SHOWN ELSEWHERE

FENCE ITEMS

| CATEGORY                            | ROADWAY         | STATION          | OFFSET    | TO | FROM     | LF        | LF    |  |    |
|-------------------------------------|-----------------|------------------|-----------|----|----------|-----------|-------|--|----|
| 0010                                | WAUKESHA BYPASS | 125+03           | 100' RT   | -  | 127+19   | 100.2' RT | 340   |  |    |
|                                     |                 | 128+50           | 25.4' RT  | -  | 129+73   | 13.8' RT  | 417   |  |    |
|                                     |                 | A 141+17         | 100' LT   | -  | A 147+06 | 124' LT   | 615   |  |    |
|                                     |                 | A 144+75         | --        | -  | A 147+00 | --        | 228   |  |    |
|                                     |                 | A 149+10         | 148.9' RT | -  | A 152+00 | 360' RT   | 483   |  |    |
|                                     |                 | A 160+25         | --        | -  | A 162+00 | --        | 181   |  |    |
|                                     |                 | A 214+97         | 175.5' RT | -  | --       | --        | 70    |  |    |
|                                     |                 | A 215+25         | 157.8' RT | -  | --       | --        | 70    |  |    |
|                                     |                 | A 243+00         | 73.0' LT  | -  | A 248+00 | 63.5' LT  | 500   |  |    |
|                                     |                 | A 244+00         | 82.5' RT  | -  | A 246+00 | 68.5' RT  | 200   |  |    |
|                                     |                 | MERRILL HILLS RD |           |    |          |           |       |  | 70 |
|                                     |                 | 83+18 19.2' LT   |           |    |          |           |       |  | -- |
| GLACIAL DRUMLIN TRAIL, N CONNECTION |                 |                  |           |    |          |           | --    |  |    |
| 29+96 11.0' RT - 31+61              |                 |                  |           |    |          |           | 157   |  |    |
| 30+14 19.3' LT - 31+61              |                 |                  |           |    |          |           | 54    |  |    |
| UNDISTRIBUTED                       |                 |                  |           |    |          |           | 190   |  |    |
| TOTALS                              |                 |                  |           |    |          |           | 2,255 |  |    |

616.0700.S

| CATEGORY | ROADWAY                    | STATION       | SPV_0090.06 |
|----------|----------------------------|---------------|-------------|
| 0010     | TEMP.GENESEE RD. CROSSOVER | 62+70 - 65+00 | 230         |
| TOTAL    |                            |               | 230         |

MISCELLANEOUS QUANTITIES

SHEET NO: 642

PROJECT NO: 2788-00-71

HWY: WAUKESHA BYPASS COUNTY: WAUKESHA PLOT NAME: by032008.mq PLOT SCALE: 1,00000:1,00000 WISDOT/CADDIS SHEET 42

TRAFFIC CONTROL ITEMS

| CATEGORY              | LOCATION              | STAGE DURATION DAYS | 643.0300 TRAFFIC CONTROL DRUMS |         | 643.0410 TRAFFIC CONTROL BARRICADES |       | 643.0420 TRAFFIC CONTROL FLEXIBLE TUBULAR MARKER |      | 643.0500 TRAFFIC CONTROL TUBULAR MARKER |        | 643.0705 TRAFFIC CONTROL WARNING LIGHTS |        | 643.0800 TRAFFIC CONTROL ARROW BOARDS |      | 643.0900 TRAFFIC CONTROL COVERING SIGNS |        | 643.0920 TRAFFIC CONTROL SIGNS FORMS |      |       |      |
|-----------------------|-----------------------|---------------------|--------------------------------|---------|-------------------------------------|-------|--------------------------------------------------|------|-----------------------------------------|--------|-----------------------------------------|--------|---------------------------------------|------|-----------------------------------------|--------|--------------------------------------|------|-------|------|
|                       |                       |                     | EACH*                          | DAYS    | EACH*                               | DAYS  | EACH*                                            | DAYS | EACH*                                   | DAYS   | EACH*                                   | DAYS   | EACH*                                 | DAYS | EACH*                                   | DAYS   | EACH*                                | DAYS | EACH* | DAYS |
| 0010                  | STAGE 1               | 8                   |                                |         |                                     |       |                                                  |      |                                         |        |                                         |        |                                       |      |                                         |        |                                      |      |       |      |
|                       | LES PAUL PKWY         |                     | 100                            | 800     |                                     |       |                                                  |      |                                         |        | 2                                       | 16     | 1                                     | 8    | 8                                       | 64     |                                      |      | 1     | 14   |
|                       | GENESSEE RD           |                     | 100                            | 800     |                                     |       |                                                  |      | 8                                       | 64     | 4                                       | 32     | 8                                     | 64   | 20                                      | 160    |                                      |      | 2     | 28   |
|                       | SAYLESVILLE RD        |                     | 30                             | 240     |                                     |       |                                                  |      | 4                                       | 32     |                                         |        |                                       |      | 10                                      | 80     |                                      |      |       |      |
|                       | MERRILL HILLS RD      |                     | 60                             | 480     |                                     |       | 35                                               | 35   |                                         |        |                                         |        |                                       |      | 30                                      | 240    |                                      |      | 2     | 28   |
|                       | STAGE 1 SUBTOTAL      |                     | 2,320                          |         | 0                                   |       | 35                                               | 35   | 112                                     |        | 64                                      |        | 8                                     |      | 584                                     |        | 0                                    |      |       |      |
|                       | STAGE 1A              | 9                   |                                |         |                                     |       |                                                  |      |                                         |        |                                         |        |                                       |      |                                         |        |                                      |      |       |      |
|                       | LES PAUL PKWY         |                     | 118                            | 1,062   | 3                                   | 27    |                                                  |      | 8                                       | 72     | 12                                      | 108    | 1                                     | 9    | 28                                      | 252    |                                      |      |       |      |
|                       | GENESSEE RD           |                     | 150                            | 1,350   |                                     |       | 16                                               | 144  | 32                                      | 288    | 12                                      | 108    | 1                                     | 9    | 39                                      | 351    |                                      |      |       |      |
|                       | SAYLESVILLE RD        |                     | 50                             | 450     |                                     |       |                                                  |      | 4                                       | 36     |                                         |        |                                       |      | 11                                      | 99     |                                      |      |       |      |
|                       | MERRILL HILLS RD      |                     | 60                             | 540     |                                     |       |                                                  |      |                                         |        |                                         |        |                                       |      | 30                                      | 270    |                                      |      |       |      |
|                       | STAGE 1 SUBTOTAL      |                     | 3,402                          |         | 27                                  |       | 16                                               | 16   | 396                                     |        | 216                                     |        | 18                                    |      | 972                                     |        | 0                                    |      |       |      |
| 277                   | STAGE 2A              |                     |                                |         |                                     |       |                                                  |      |                                         |        |                                         |        |                                       |      |                                         |        |                                      |      |       |      |
|                       | LES PAUL PKWY         |                     | 76                             | 21,052  | 3                                   | 831   | 43                                               | 43   | 24                                      | 6,648  | 12                                      | 3,324  | 1                                     | 277  | 20                                      | 5,540  |                                      |      | 1     | 14   |
|                       | GENESSEE RD           |                     | 140                            | 38,780  |                                     |       | 143                                              | 143  | 24                                      | 6,648  | 25                                      | 6,925  | 1                                     | 277  | 53                                      | 14,681 |                                      |      | 2     | 28   |
|                       | SAYLESVILLE RD        |                     | 10                             | 2,770   |                                     |       | 48                                               | 48   |                                         |        |                                         |        |                                       |      | 13                                      | 3,601  |                                      |      |       |      |
|                       | SUNSET DRIVE          |                     |                                |         |                                     |       |                                                  |      | 45                                      | 12,465 |                                         |        |                                       |      | 12                                      | 3,324  |                                      |      |       |      |
|                       | MERRILL HILLS RD      |                     | 230                            | 63,710  | 1                                   | 277   |                                                  |      | 20                                      | 5,540  | 28                                      | 7,756  | 1                                     | 277  | 76                                      | 21,052 |                                      |      | 1     | 14   |
|                       | MACARTHUR RD          |                     |                                |         |                                     |       |                                                  |      |                                         |        |                                         |        |                                       |      | 3                                       | 831    |                                      |      |       |      |
|                       | KAME TER              |                     |                                |         |                                     |       |                                                  |      |                                         |        |                                         |        |                                       |      | 2                                       | 554    |                                      |      |       |      |
|                       | MADISON ST            |                     |                                |         |                                     |       |                                                  |      | 10                                      | 2,770  |                                         |        |                                       |      | 9                                       | 2,493  |                                      |      |       |      |
|                       | KISON HILL DR         |                     |                                |         |                                     |       |                                                  |      |                                         |        |                                         |        |                                       |      | 1                                       | 277    |                                      |      |       |      |
|                       | SUNMIT AVE            |                     |                                |         |                                     |       |                                                  |      |                                         |        |                                         |        |                                       |      | 6                                       | 1,662  |                                      |      | 1     | 14   |
|                       |                       | STAGE 2A SUBTOTAL   |                                | 126,312 |                                     | 1,108 |                                                  | 234  | 234                                     | 34,071 |                                         | 18,005 |                                       | 831  |                                         | 54,015 |                                      | 0    |       |      |
| 74                    | STAGE 2B              |                     |                                |         |                                     |       |                                                  |      |                                         |        |                                         |        |                                       |      |                                         |        |                                      |      |       |      |
|                       | SUNSET DR             |                     |                                |         |                                     |       |                                                  |      | 24                                      | 1,776  |                                         |        |                                       |      | 9                                       | 666    |                                      |      |       |      |
|                       | STAGE 2B SUBTOTAL     |                     | 0                              |         | 0                                   |       | 0                                                | 0    | 1,776                                   |        | 0                                       |        | 0                                     |      | 666                                     |        | 0                                    |      |       |      |
| 17                    | STAGE 2C              |                     |                                |         |                                     |       |                                                  |      |                                         |        |                                         |        |                                       |      |                                         |        |                                      |      |       |      |
|                       | GENESSEE RD           |                     | 95                             | 1,615   |                                     |       |                                                  |      | 45                                      | 765    |                                         |        |                                       |      | 27                                      | 459    |                                      |      |       |      |
|                       | STAGE 2C SUBTOTAL     |                     | 1,615                          |         | 0                                   |       | 0                                                | 0    | 765                                     |        | 0                                       |        | 0                                     |      | 459                                     |        | 0                                    |      |       |      |
| 74                    | STAGE 2D              |                     |                                |         |                                     |       |                                                  |      |                                         |        |                                         |        |                                       |      |                                         |        |                                      |      |       |      |
|                       | GENESSEE RD           |                     | 98                             | 7,252   | 2                                   | 148   | 135                                              | 135  | 50                                      | 3,700  | 30                                      | 2,220  | 1                                     | 74   | 55                                      | 4,070  |                                      |      |       |      |
|                       | LES PAUL PKWY         |                     | 75                             | 5,550   | 1                                   | 74    | 54                                               | 54   | 10                                      | 740    | 16                                      | 1,184  | 1                                     | 74   | 10                                      | 740    |                                      |      |       |      |
|                       | SAYLESVILLE RD        |                     | 30                             | 2,220   |                                     |       |                                                  |      | 10                                      | 740    |                                         |        |                                       |      | 6                                       | 444    |                                      |      |       |      |
|                       | STAGE 2D SUBTOTAL     |                     | 15,022                         |         | 222                                 |       | 189                                              | 189  | 5,180                                   |        | 3,404                                   |        | 148                                   |      | 5,254                                   |        | 0                                    |      |       |      |
| 44                    | STAGE 2E              |                     |                                |         |                                     |       |                                                  |      |                                         |        |                                         |        |                                       |      |                                         |        |                                      |      |       |      |
|                       | WAUKESHA BYPASS       |                     | 30                             | 1,320   | 4                                   | 176   |                                                  |      | 60                                      | 2,640  |                                         |        |                                       |      | 28                                      | 1,232  |                                      |      |       |      |
|                       | SUNSET DR             |                     |                                |         |                                     |       |                                                  |      |                                         |        |                                         |        |                                       |      | 6                                       | 264    |                                      |      |       |      |
|                       | MERRILL HILLS RD      |                     |                                |         |                                     |       |                                                  |      |                                         |        |                                         |        |                                       |      | 17                                      | 748    |                                      |      |       |      |
|                       | MACARTHUR RD          |                     |                                |         |                                     |       |                                                  |      |                                         |        |                                         |        |                                       |      | 2                                       | 88     |                                      |      |       |      |
|                       | KAME TER (STAGE 2E-1) |                     |                                |         |                                     |       |                                                  |      | 4                                       | 176    |                                         |        |                                       |      | 11                                      | 484    |                                      |      |       |      |
|                       | KAME TER (STAGE 2E-2) |                     |                                |         |                                     |       |                                                  |      | 4                                       | 176    |                                         |        |                                       |      | 2                                       | 88     |                                      |      |       |      |
| MADISON ST            |                       |                     |                                |         |                                     |       |                                                  | 8    | 352                                     |        |                                         |        |                                       | 2    | 88                                      |        |                                      |      |       |      |
| GLACIAL DRUMLIN TRAIL |                       |                     |                                |         |                                     |       |                                                  | 4    | 176                                     |        |                                         |        |                                       | 2    | 88                                      |        |                                      |      |       |      |
|                       | STAGE 2E SUBTOTAL     |                     | 1,320                          |         | 176                                 |       | 0                                                | 0    | 3,520                                   |        | 0                                       |        | 0                                     |      | 3,256                                   |        | 0                                    |      |       |      |

MISCELLANEOUS QUANTITIES

HWY: WAUKESHA BYPASS

COUNTY: WAUKESHA

PROJECT NO: 2788-00-71

SHEET NO: 694

TRAFFIC CONTROL ITEMS (CONTINUED)

| CATEGORY | LOCATION                  | STAGE DURATION DAYS | 643.0300 TRAFFIC CONTROL DRUMS |         | 643.0410 TRAFFIC CONTROL BARRICADES |       | 643.0420 TRAFFIC CONTROL FLEXIBLE TUBULAR MARKER |        | 643.0705 TRAFFIC CONTROL WARNING LIGHTS |       | 643.0715 TRAFFIC CONTROL ARROW/BOARDS |        | 643.0800 TRAFFIC CONTROL COVERING SIGNS |        | 643.0900 TRAFFIC CONTROL SIGNS |       | 643.0920 TRAFFIC CONTROL SIGNS |        |       |
|----------|---------------------------|---------------------|--------------------------------|---------|-------------------------------------|-------|--------------------------------------------------|--------|-----------------------------------------|-------|---------------------------------------|--------|-----------------------------------------|--------|--------------------------------|-------|--------------------------------|--------|-------|
|          |                           |                     | EACH*                          | DAYS    | EACH*                               | DAYS  | EACH*                                            | DAYS   | EACH*                                   | DAYS  | EACH*                                 | DAYS   | EACH*                                   | DAYS   | EACH*                          | DAYS  | EACH*                          | DAYS   | EACH* |
| 0010     | STAGE 3                   | 56                  |                                |         |                                     |       |                                                  |        |                                         |       |                                       |        |                                         |        |                                |       |                                |        |       |
|          | WAUKESHA BYPASS           |                     | 330                            | 18,480  | 2                                   | 112   | 60                                               | 3,360  | 685                                     | 685   | 90                                    | 5,040  | 50                                      | 2,800  | 1                              | 56    | 100                            | 5,600  |       |
|          | GENESSEE RD               |                     | 95                             | 5,320   | 1                                   | 56    | 12                                               | 672    | 126                                     | 126   | 18                                    | 1,008  | 30                                      | 1,680  | 1                              | 56    | 38                             | 2,128  |       |
|          | SAYLESVILLE RD            |                     | 25                             | 1,400   | 1                                   | 56    | 2                                                | 112    |                                         |       | 4                                     | 224    |                                         |        |                                | 10    | 560                            |        |       |
|          | SUNSET DR                 |                     |                                |         |                                     |       | 3                                                | 168    |                                         |       | 6                                     | 336    |                                         |        |                                | 10    | 560                            |        |       |
|          | MACARTHUR RD              |                     |                                |         |                                     |       | 2                                                | 112    |                                         |       | 4                                     | 224    |                                         |        |                                | 3     | 168                            |        |       |
|          | KAME TER                  |                     |                                |         |                                     |       |                                                  |        |                                         |       |                                       |        |                                         |        |                                | 2     | 112                            |        |       |
|          | MADISON ST                |                     |                                |         |                                     |       | 2                                                | 112    | 35                                      | 35    | 4                                     | 224    | 20                                      | 1,120  |                                | 13    | 728                            |        |       |
|          | KSDON HILL DR (STAGE 3-1) |                     | 20                             | 1,120   |                                     |       | 1                                                | 56     |                                         |       | 2                                     | 112    |                                         |        |                                | 4     | 224                            |        |       |
|          | KSDON HILL DR (STAGE 3-2) |                     | 20                             | 1,120   |                                     |       | 1                                                | 56     |                                         |       | 2                                     | 112    |                                         |        |                                | 7     | 392                            |        |       |
|          | KSDON HILL DR (STAGE 3-3) |                     | 20                             | 1,120   |                                     |       | 1                                                | 56     |                                         |       | 2                                     | 112    |                                         |        |                                |       |                                |        |       |
|          | SUMMIT AVE                |                     |                                |         |                                     |       |                                                  |        |                                         |       |                                       |        |                                         |        |                                | 11    | 616                            |        |       |
|          | STAGE 3 SUBTOTAL          |                     |                                | 28,560  |                                     | 224   |                                                  | 4,704  | 846                                     | 846   |                                       | 7,392  |                                         | 5,600  |                                | 112   |                                | 11,088 | 0     |
|          | STAGE 4                   | 21                  |                                |         |                                     |       |                                                  |        |                                         |       |                                       |        |                                         |        |                                |       |                                |        |       |
|          | WAUKESHA BYPASS           |                     | 225                            | 4,725   | 3                                   | 63    | 12                                               | 252    |                                         |       | 24                                    | 504    | 55                                      | 1,155  | 4                              | 84    | 52                             | 1,092  |       |
|          | GENESSEE RD               |                     | 100                            | 2,100   |                                     |       | 9                                                | 189    |                                         |       | 18                                    | 378    | 24                                      | 504    | 2                              | 42    | 22                             | 462    |       |
|          | SAYLESVILLE RD            |                     |                                |         |                                     |       |                                                  |        |                                         |       |                                       |        |                                         |        |                                | 2     | 42                             |        |       |
|          | SUNSET DR                 |                     |                                |         |                                     |       |                                                  |        |                                         |       |                                       |        |                                         |        |                                | 4     | 84                             |        |       |
|          | KAME TER                  |                     |                                |         |                                     |       |                                                  |        |                                         |       |                                       |        |                                         |        |                                | 2     | 42                             |        |       |
|          | MADISON ST                |                     |                                |         |                                     |       |                                                  |        |                                         |       |                                       |        |                                         |        |                                | 6     | 126                            |        |       |
|          | KSDON HILL DR             |                     |                                |         |                                     |       |                                                  |        |                                         |       |                                       |        |                                         |        |                                | 2     | 42                             |        |       |
|          | SUMMIT AVE                |                     |                                |         |                                     |       |                                                  |        |                                         |       |                                       |        |                                         |        |                                | 6     | 126                            |        |       |
|          | STAGE 4 SUBTOTAL          |                     |                                | 7,455   |                                     | 63    |                                                  | 441    | 0                                       | 0     |                                       | 882    |                                         | 1,659  |                                | 126   |                                | 2,016  | 0     |
|          | UNDISTRIBUTED             |                     |                                | 14,984  |                                     | 180   |                                                  | 3,286  | 40                                      | 40    |                                       | 5,406  |                                         | 2,952  |                                | 137   |                                | 7,190  | 60    |
|          | TOTALS                    |                     |                                | 201,000 |                                     | 2,000 |                                                  | 37,000 | 1,360                                   | 1,360 |                                       | 59,500 |                                         | 31,900 |                                | 1,380 |                                | 85,500 | 9     |

\* ADDITIONAL QUANTITIES SHOWN ELSEWHERE  
 \*\* FOR INFORMATION ONLY

TRAFFIC CONTROL

| CATEGORY | STAGE | LOCATION | 643.5000 EACH |
|----------|-------|----------|---------------|
| 0010     | ALL   | PROJECT  | 1             |
| TOTAL    |       |          | 1             |

Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 695  
 July 2, 2018

Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 697  
 July 2, 2018

3

TRAFFIC CONTROL DETOUR SIGNING ITEMS

| CATEGORY                            | STAGE          | STAGE DURATION DAYS | REMOVING SIGNS TYPE II EACH | COVERING SIGNS TYPE II EACH | DETOUR SIGN NUMBER** | SIGN DIMENSIONS FT**   | NO. OF SIGNS EACH** | SIGN AREA SF** | 643.1000 TRAFFIC CONTROL SIGNS |               | NO. OF SIGNS EACH** | SIGN AREA SF**                 | SIGN TOTAL SF | REMARKS |
|-------------------------------------|----------------|---------------------|-----------------------------|-----------------------------|----------------------|------------------------|---------------------|----------------|--------------------------------|---------------|---------------------|--------------------------------|---------------|---------|
|                                     |                |                     |                             |                             |                      |                        |                     |                | FIXED MESSAGE SIGNS            | CONTROL SIGNS |                     |                                |               |         |
| 0010                                | STAGE 2B       | 74                  | --                          | 4                           | 95                   | 6.5 x 3.0<br>2.5 x 2.0 | 3                   | 19.50<br>5.00  | 56.50<br>275.00                | 200           | 14,800              | 643.1000 TRAFFIC CONTROL SIGNS | 200           | 14,800  |
|                                     | STAGE 2C       | 17                  | --                          | 11                          | 36<br>37             | 7.5 x 3.0<br>7.5 x 3.0 | 4<br>1              | 22.50<br>22.50 | 90.00<br>22.50                 | 148           | 2,516               | 643.0900 TRAFFIC CONTROL SIGNS | 148           | 2,516   |
|                                     | STAGE 2D       | 74                  | 3                           | 6                           | 17                   | 8.0 x 3.0              | 2                   | 24.00          | 48.00                          | 212           | 15,688              |                                | 212           | 15,688  |
|                                     | STAGE 2E       | 44                  | 3                           | --                          | 79                   | 7.0 x 3.0<br>3.0 x 2.5 | 1<br>49             | 21.00<br>7.50  | 21.00<br>367.50                | 106           | 4,664               |                                | 106           | 4,664   |
| <u>GLACIAL DRUMLIN TRAIL DETOUR</u> |                |                     |                             |                             |                      |                        |                     |                |                                |               |                     |                                |               |         |
|                                     | STAGE 2E       | 44                  | --                          | --                          | --                   | 3.0 x 2.5              | 12                  | 7.50           | 90.00                          | 26            | 1,144               |                                | 26            | 1,144   |
|                                     | STAGE 3        | 56                  | --                          | --                          | --                   | 3.0 x 2.5              | 12                  | 7.50           | 90.00                          | 26            | 1,456               |                                | 26            | 1,456   |
|                                     | TRUCK ROUTE    | 381                 | --                          | --                          | 9                    | 11.5 x 4.5             | 4                   | 51.75          | 207.00                         | 102           | 38,862              |                                | 102           | 38,862  |
|                                     | TRUCK ROUTE 2B | 74                  | --                          | --                          | --                   | 11.5 x 4.5             | 3                   | 51.75          | 155.25                         | 142           | 10,508              |                                | 142           | 10,508  |
|                                     | UNDISTRIBUTED  |                     |                             |                             |                      |                        |                     |                | 7.25                           |               | 862                 |                                |               | 862     |
| TOTALS                              |                |                     | 6                           | 21                          |                      |                        |                     |                | 1,432.00                       |               | 90,500              |                                |               | 90,500  |

\* ADDITIONAL QUANTITIES SHOWN ELSEWHERE  
 \*\* FOR INFORMATION ONLY

3

TRAFFIC CONTROL SIGNS FIXED MESSAGE

| CATEGORY | LOCATION              | SIGN DIMENSIONS FT** | SIGN OF EACH** | SIGN AREA SF** | SIGN AREA SF | SIGN TOTAL SF | REMARKS  |
|----------|-----------------------|----------------------|----------------|----------------|--------------|---------------|----------|
|          |                       |                      |                |                |              |               |          |
| 0010     | GENESEERD             | 7.5 X 2.0            | 2              | 15.00          | 30.00        | 30.00         | STAGE 1  |
|          | MERRILL HILLS ROAD    | 6.0 X 3.0            | 1              | 18.00          | 18.00        | 18.00         | STAGE 2E |
|          | GLACIAL DRUMLIN TRAIL | 8.0 X 4.0            | 2              | 32.00          | 64.00        | 64.00         | STAGE 2E |
| TOTAL    |                       |                      |                |                |              | 18.00         |          |

\* ADDITIONAL QUANTITIES SHOWN ELSEWHERE  
 \*\* FOR INFORMATION ONLY

CONCRETE BARRIER & CRASH CUSHIONS TEMPORARY

| CATEGORY | STAGE | ROADWAY        | STA - STA     | DELIVERED |              | STATION | BACK WIDTH | OBJECT MARKING PATTERN | CRASH TEST LEVEL | TRAFFIC DIRECTION | TRAFFIC LOCATION | CRASH CUSHION SHIELDS   |
|----------|-------|----------------|---------------|-----------|--------------|---------|------------|------------------------|------------------|-------------------|------------------|-------------------------|
|          |       |                |               | LF        | INSTALLED LF |         |            |                        |                  |                   |                  |                         |
| 0010     | 3-1   | KISSON HILL DR | 50+90 - 53+90 | --        | --           | 1       | 4          | OM-3L                  | TL-3             | UNIDIRECTIONAL    | R                | TEMPORARY CONC. BARRIER |
|          |       | KISSON HILL DR | 50+90 - 53+90 | 300       | 300          | --      | --         |                        |                  |                   |                  |                         |
| TOTALS   |       |                |               | 300       | 300          | 1       |            |                        |                  |                   |                  |                         |

\*\* FOR INFORMATION ONLY

MARKING REMOVAL LINE 4-INCH

| CATEGORY | LOCATION   | STATION       | SIGN AREA SF | SIGN TOTAL SF |
|----------|------------|---------------|--------------|---------------|
|          |            |               |              |               |
| 0010     | GENESEERD  | 34+11 - 61+84 | 646.9000     | 646.9000      |
|          | MADISON ST | 48+00 - 53+97 | 250          | 250           |
| TOTAL    |            |               |              | 2,450         |

PROJECT NO: 2788-00-71

HWY: WAUKESHA BYPASS

COUNTY: WAUKESHA

MISCELLANEOUS QUANTITIES

SHEET NO: 697



**LEGEND**

- (A1) ASPHALTIC SURFACE 3-INCH
- (A2) ASPHALTIC SURFACE, 5-INCH
- (B1) BASE AGGREGATE DENSE 3/4-INCH
- (B2) BASE AGGREGATE DENSE 1 1/4-INCH
- (C1) CONCRETE CURB & GUTTER 30-INCH TYPE A
- (C2) CONCRETE CURB & GUTTER 30-INCH TYPE D
- (C3) CONCRETE CURB & GUTTER 6-INCH SLOPED 30-INCH TYPE G
- (C4) CONCRETE CURB & GUTTER 6-INCH SLOPED 30-INCH TYPE J
- (C5) CONCRETE CURB & GUTTER 6-INCH SLOPED 36-INCH TYPE D
- (C6) CONCRETE CURB PEDESTRIAN
- (C7) CONCRETE CURB & GUTTER 6-INCH SLOPED 30-INCH TYPE G SPECIAL
- (C8) CONCRETE CURB & GUTTER 4-INCH SLOPED 30-INCH TYPE G
- (C9) CONCRETE CURB & GUTTER 4-INCH SLOPED 30-INCH TYPE J
- (C10) CONCRETE CURB TYPE A
- (C11) CONCRETE CURB TYPE D
- (D1) CONCRETE DRIVEWAY 6-INCH
- (D2) CONCRETE DRIVEWAY 7-INCH
- (D3) ASPHALTIC SURFACE DRIVEWAYS & FIELD ENTRANCES, 3-INCH
- (D4) ASPHALTIC SURFACE DRIVEWAYS & FIELD ENTRANCES, 4-INCH
- (D5) ASPHALTIC SURFACE DRIVEWAYS & FIELD ENTRANCES, 6-INCH
- (D6) AGGREGATE DRIVEWAY, BASE AGGREGATE DENSE 3/4-INCH, 6-INCH
- (E1) CONCRETE SURFACE DRAIN
- (E2) ASPHALTIC FLUME
- (F1) WALL R-67-145 FENCE CHAIN LINK POLYMER COATED 4-FT
- (F2) FENCE CHAIN LINK POLYMER COATED 4-FT
- (G1) MGS GUARDRAIL 3
- (G2) MGS THREE BEAM TRANSITION
- (G3) MGS GUARDRAIL TERMINAL EAT
- (G4) GUARDRAIL MOW STRIP ASPHALT
- (G5) MGS GUARDRAIL TERMINAL TYPE 2
- (G6) STEEL PLATE BEAM GUARD CLASS A
- (G7) STEEL PLATE BEAM GUARD ENERGY ABSORBING TERMINAL
- (G8) ANCHORAGES FOR STEEL PLATE BEAM GUARD TYPE 2
- (G9) STEEL PLATE BEAM GUARD CLASS A TYPE LHW
- (G10) MGS GUARDRAIL LONG SPAN (MGS L)

- (H1) HMA PAVEMENT 4-INCH
- (H2) HMA PAVEMENT 5-INCH
- (H3) HMA PAVEMENT 5.5-INCH
- (H4) HMA PAVEMENT 8-INCH
- (K1) CONCRETE PAVEMENT 8-INCH, DOMELED AND TINED
- (M1) CONCRETE MEDIAN SLOPED NOSE TYPE 1
- (M2) CONCRETE MEDIAN SLOPED NOSE TYPE 2
- (M3) MAINTENANCE OPENING
- (P1) CONCRETE PAVEMENT APPROACH SLAB
- (T1) CONCRETE BARRIER TYPE S36
- (T2) CONCRETE BARRIER TYPE S36A
- (T3) CONCRETE BARRIER TRANSITION
- (T4) CONCRETE BARRIER TYPE S42C
- (T5) END ANCHOR SINGLE SLOPE CONCRETE BARRIER
- (W1) CONCRETE SIDEWALK 5-INCH
- (W2) CONCRETE CURB RAMP TYPE 2
- (W3) CONCRETE CURB RAMP TYPE 3
- (W4) CONCRETE CURB RAMP TYPE 4B1
- (W5) CONCRETE CURB RAMP TYPE 5
- (W6) CONCRETE CURB RAMP TYPE 6
- (W7) CONCRETE CURB RAMP TYPE 7B
- (W8) CONCRETE CURB RAMP TYPE 8
- (W9) CONCRETE SIDEWALK 7-INCH

**NOTES**

- 1) DIMENSIONS SHOWN ARE TO FLANGE OR LANE LINES.
- 2) TAPER CALLOUTS ARE TO FLANGE LINES OR EDGE OF PAVEMENT.
- 3) CURB & GUTTER RADI CALLOUTS ARE TO FLANGE LINE.
- 4) ALL CURB RAMPS REQUIRE YELLOW DETECTABLE WARNING FIELDS.
- 5) SEE CURB RAMP CONSTRUCTION DETAILS FOR ADDITIONAL ELEVATION AND LAYOUT INFORMATION AT CURB RAMPS.
- 6) VERIFY EXISTING ELEVATIONS AT MATCHLINES IN THE FIELD.

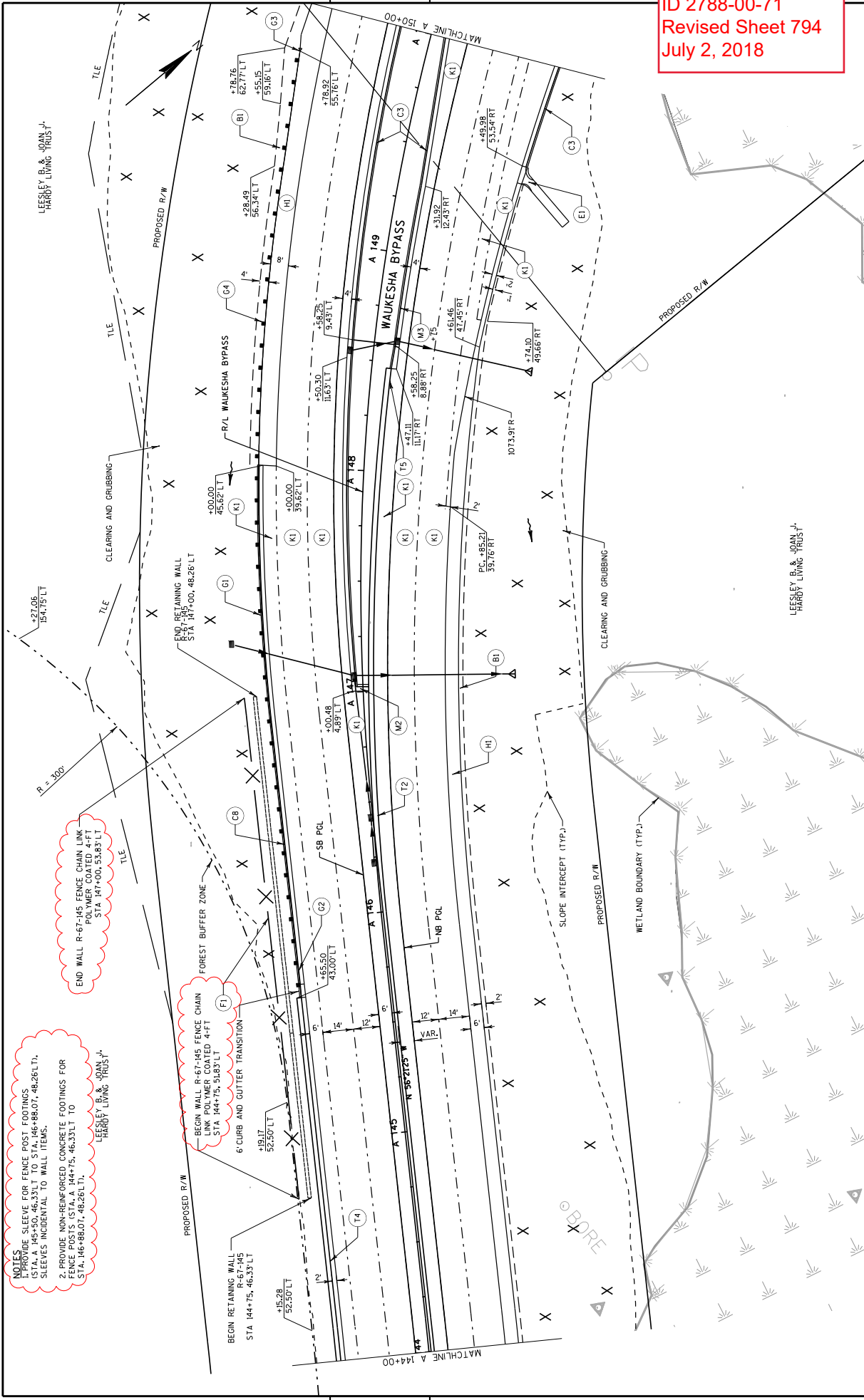
- HES CONCRETE AREAS
- PROPOSED INLET
- PROPOSED MANHOLE
- DRIVEWAY CULVERTS
- CURB RAMP DETECTABLE WARNING FIELD YELLOW
- REVERSE SLOPE GUTTER
- ROADWAY GUARDRAIL
- X X X X SAWING ASPHALT
- X X X X SAWING CONCRETE
- X CLEARING AND GRUBBING
- FENCE

**REMOVAL LEGEND**

- X CLEARING AND GRUBBING
- REMOVING PAVEMENT
- REMOVING CURB AND GUTTER
- REMOVING CONCRETE SIDEWALK
- REMOVING FENCE
- REMOVING GUARDRAIL
- REMOVING MANHOLES
- REMOVING INLETS/CATCH BASINS
- OBLITERATE OLD ROAD

Addendum No. 01  
ID 2788-00-71  
Revised Sheet 769  
July 2, 2018

Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 794  
 July 2, 2018



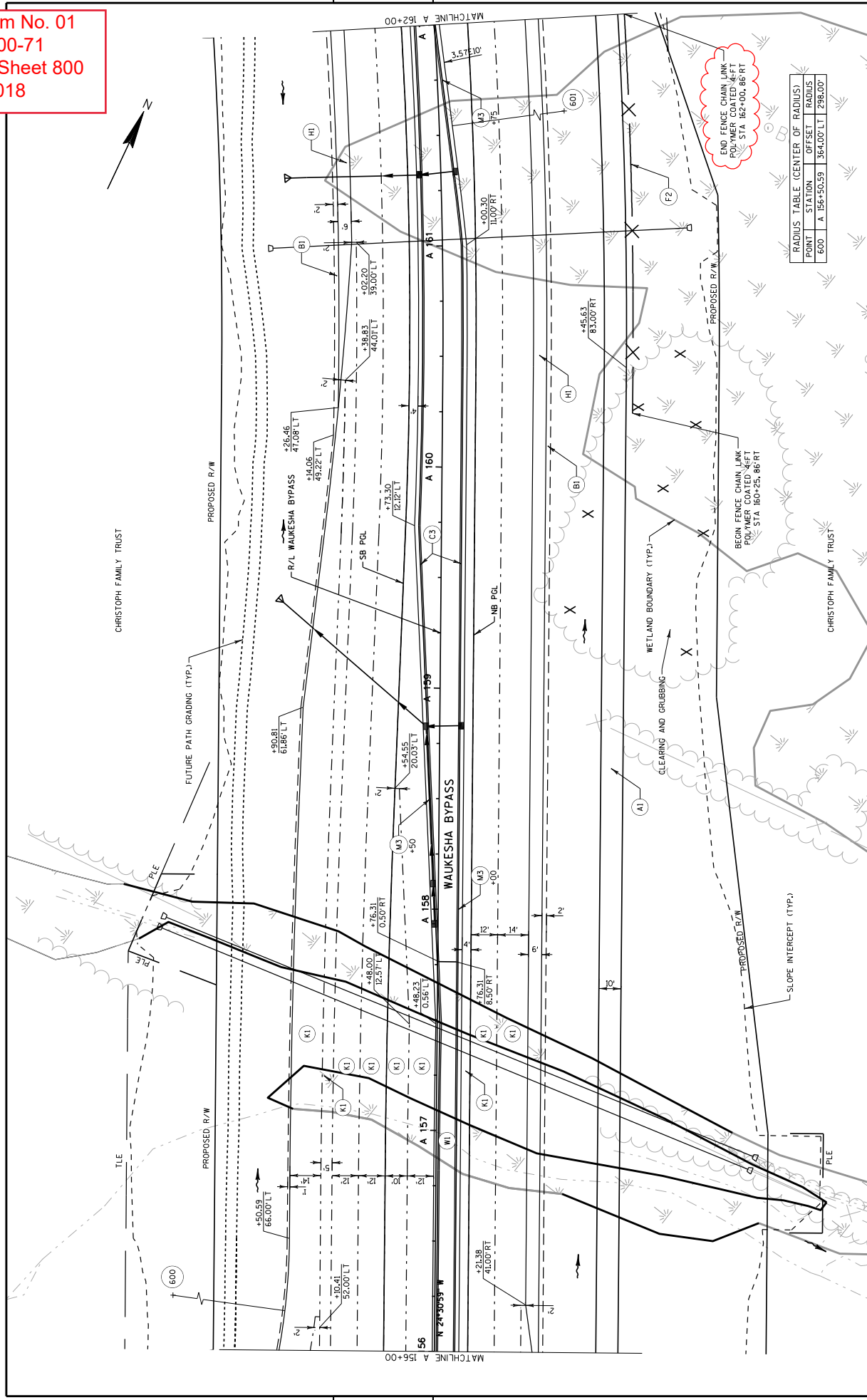
**NOTES**  
 1. PROVIDE SLEEVE FOR FENCE POST FOOTINGS. SLEEVE SHALL BE 1/2" DIA. POLYMER COATED 4-FT. SLEEVES INCIDENTAL TO WALL ITEMS.  
 2. PROVIDE NON-REINFORCED CONCRETE FOOTINGS FOR FENCE POSTS (STA. A 144+75, 46.33' LT TO STA. 146+86.07, 46.26' LT).  
 LEESLEY, B. & JOAN, J.  
 HARDY LIVING TRUST

END WALL R-67-145 FENCE CHAIN LINK POLYMER COATED 4-FT STA 147+00, 53.83' LT  
 R = 300'

BEGIN WALL R-67-145 FENCE CHAIN LINK POLYMER COATED 4-FT STA 144+75, 46.33' LT  
 6' CURB AND GUTTER TRANSITION  
 F1

|                                                          |                      |                  |                       |           |
|----------------------------------------------------------|----------------------|------------------|-----------------------|-----------|
| PROJECT NO: 2788-00-71                                   | HWY: WAUKESHA BYPASS | COUNTY: WAUKESHA | PLAN: WAUKESHA BYPASS | SHEET 794 |
| FILE NAME : \\m:\w00\lmg\proj\59971\1\cadd\by_080125.dwg |                      |                  |                       |           |
| PLOT DATE : 6/28/2018 1:37:49 PM                         |                      |                  |                       |           |
| PLOT BY : lzlcdek                                        |                      |                  |                       |           |
| PLOT NAME :                                              |                      |                  |                       |           |
| PLOT SCALE : 40:1                                        |                      |                  |                       |           |
| WISDOT/CADD SHEET 44                                     |                      |                  |                       |           |

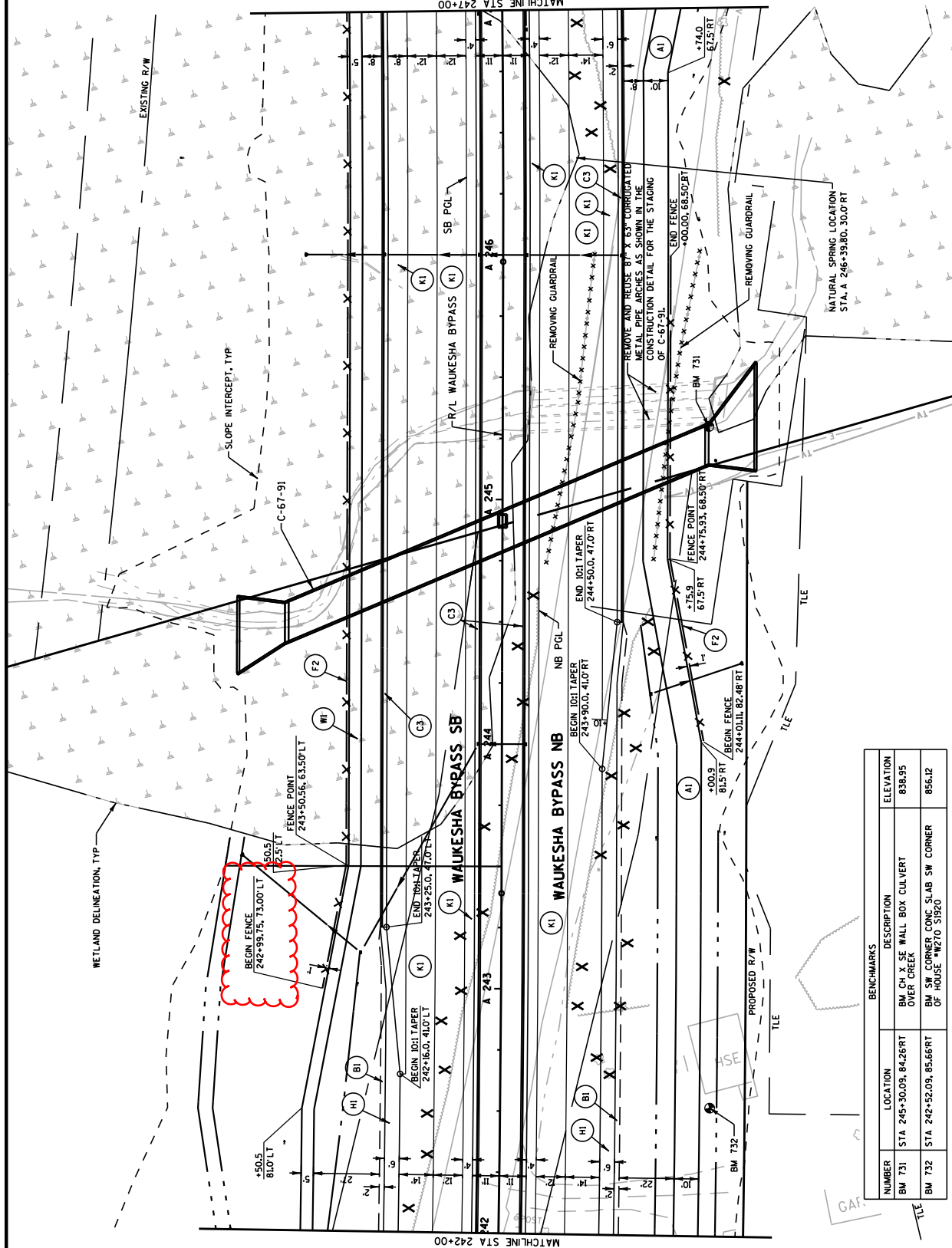
Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 800  
 July 2, 2018



| RADIUS TABLE (CENTER OF RADIUS) |             |            |         |
|---------------------------------|-------------|------------|---------|
| POINT                           | STATION     | OFFSET     | RADIUS  |
| 600                             | A 156+50.59 | 364.00' LT | 298.00' |



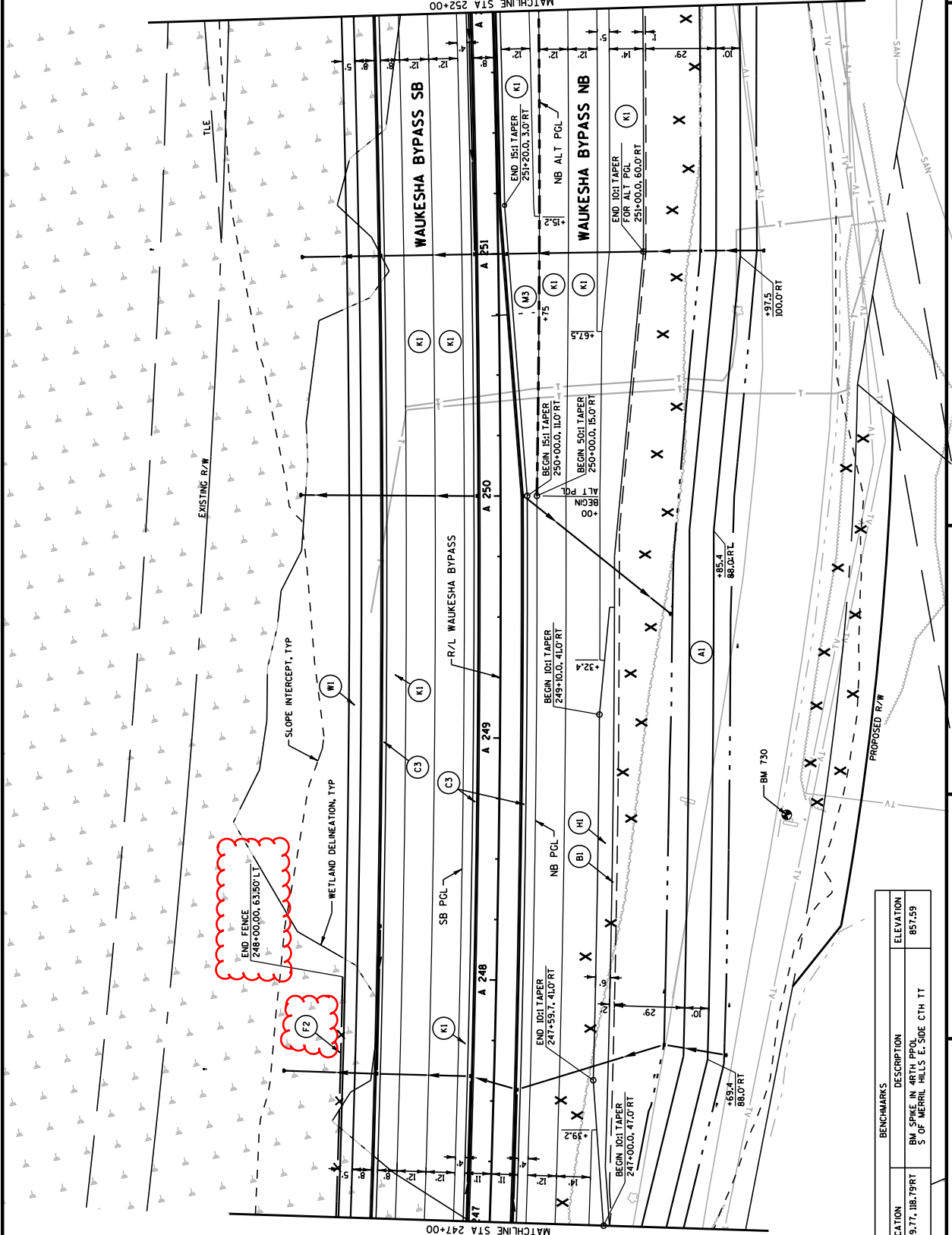
Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 837  
 July 2, 2018



| BENCHMARKS |                         |                                                       |           |
|------------|-------------------------|-------------------------------------------------------|-----------|
| NUMBER     | LOCATION                | DESCRIPTION                                           | ELEVATION |
| BM 731     | STA 245+30.09, 84.26 RT | BM CV SE WALL BOX CULVERT OVER CREEK                  | 838.95    |
| BM 732     | STA 242+52.09, 85.66 RT | BM SW CORNER CONC SLAB SW CORNER OF HOUSE #1210 35920 | 856.12    |



Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 840  
 July 2, 2018



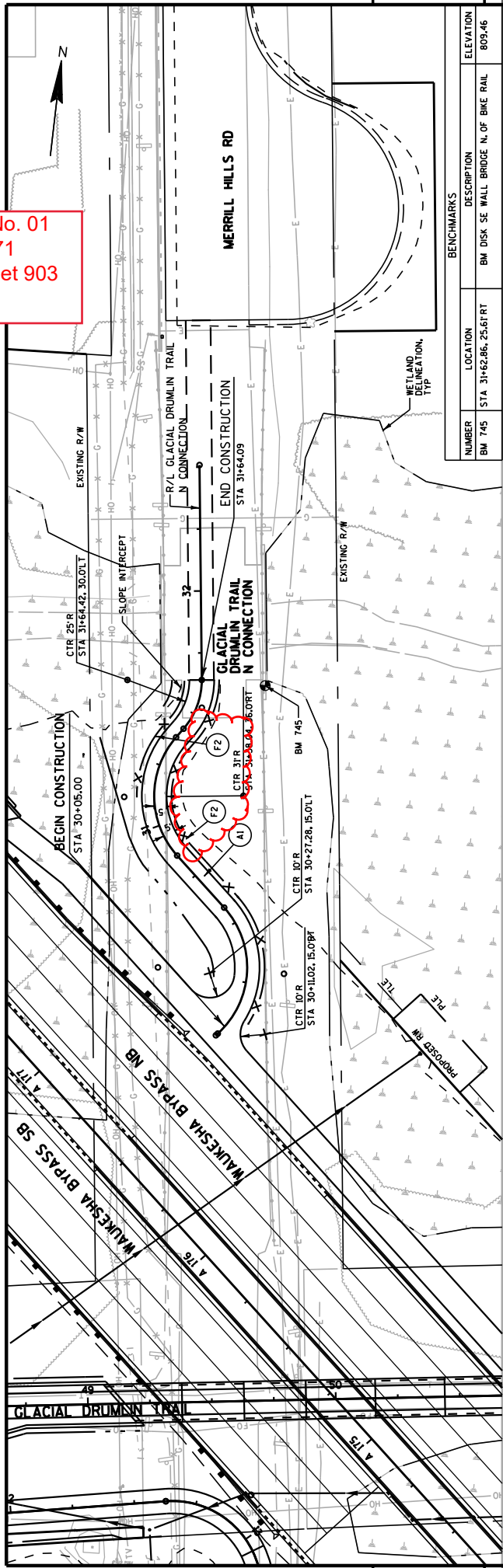
| NUMBER | LOCATION                 | DESCRIPTION                                                  | ELEVATION |
|--------|--------------------------|--------------------------------------------------------------|-----------|
| BM 730 | STA 248+63.77, 118.79 RT | BM SPINE IN NORTH POOL<br>5' OF MERRILL HILLS E. SIDE CTH TT | 851.59    |

PROJECT NO: 2788-00-71  
 COUNTY: WAUKESHA  
 PLAN: WAUKESHA BYPASS  
 SHEET 840  
 HWY: WAUKESHA BYPASS  
 PLOT DATE : 27-JUN-2018 17:32  
 PLOT BY : v101.r1  
 PLOT SCALE : 60.181818:1  
 WISDOT/CADDIS SHEET 44

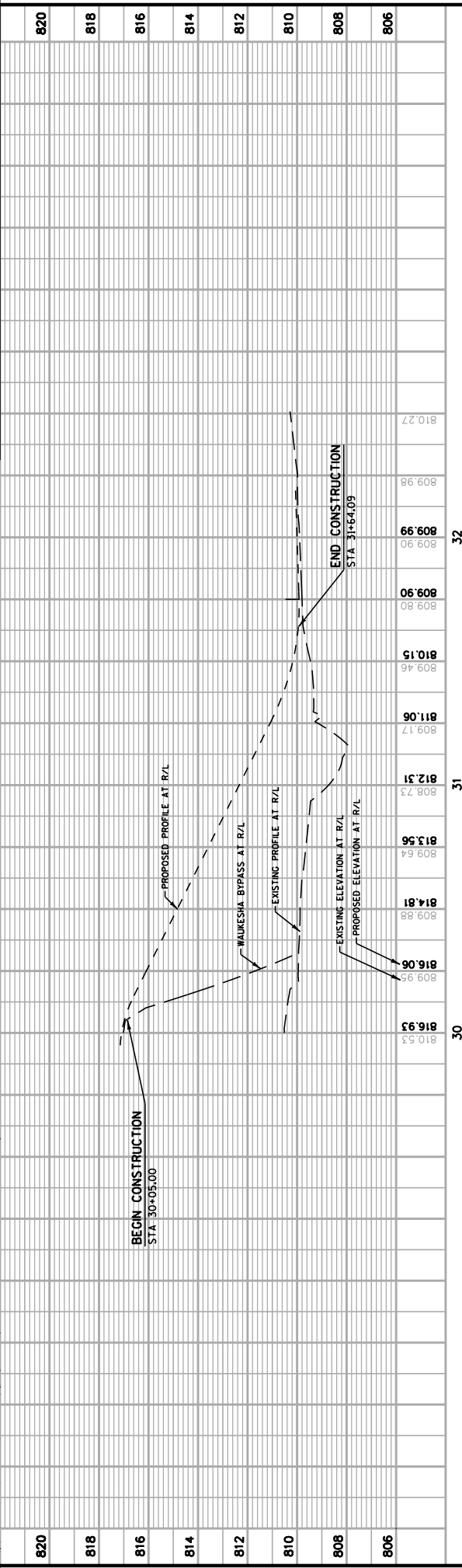
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Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 903  
 July 2, 2018



| NUMBER | LOCATION               | DESCRIPTION                            | ELEVATION |
|--------|------------------------|----------------------------------------|-----------|
| BM 745 | STA 31+62.86, 25.61 FT | BM DISK SE WALL BRIDGE N. OF BIKE RAIL | 809.46    |



| STATION | ELEVATION |
|---------|-----------|
| 30+93   | 816.93    |
| 30+95   | 816.06    |
| 30+98   | 814.81    |
| 30+64   | 813.56    |
| 30+73   | 812.31    |
| 30+17   | 811.06    |
| 30+46   | 809.46    |
| 30+80   | 809.80    |
| 30+90   | 809.90    |
| 30+96   | 809.96    |
| 30+98   | 809.98    |
| 31+02   | 810.27    |
| 31+08   | 810.15    |
| 31+15   | 810.15    |
| 31+27   | 810.27    |

**DESIGN DATA**

STATE PROJECT NUMBER  
**2788-00-71**

LIVE LOAD:  
DESIGN WINDING: HL 93  
OPERATING RATING: RF = 113  
WISCONSIN STANDARD PERMIT VEHICLE (WIS-SPV): 250(KIPS)  
STRUCTURE IS DESIGNED FOR A FUTURE WEARING SURFACE OF 20 POUNDS PER SQUARE FOOT.

MATERIAL PROPERTIES:  
CONCRETE MASONRY:  
SUPERSTRUCTURE & STRUCTURAL APPROACH SLAB —  $f_c = 4,000$  P.S.I.  
ALL OTHER —  $f_c = 3,500$  P.S.I.  
BAR STEEL REINFORCEMENT:  
STAINLESS, GRADE 60 —  $f_y = 60,000$  P.S.I.  
45W PRESTRESSED ORDERS:  
TENSILE STRENGTH —  $f_t = 8,000$  P.S.I.  
STRANDS: 0.6" DIA. WITH ULTIMATE TENSILE STRENGTH OF 270,000 P.S.I.

**FOUNDATION DATA**

ABUTMENTS TO BE SUPPORTED ON HP 12-INCH X 63 PILING DRIVEN TO A REQUIRED DRIVING RESISTANCE OF 220 TONS \* PER PILE AS DETERMINED BY THE MODIFIED GATES DYNAMIC FORMULA. \* PERCENTAGE OF PILING TO BE DRIVEN TO FULL CAPACITY AND 85'-0" LONG AT NORTH ABUTMENT, PILE POINTS REQUIRED.

\* THE FACTORED AXIAL RESISTANCE OF PILES IN COMPRESSION USED FOR DESIGN IS THREE TIMES THE REQUIRED DRIVING RESISTANCE MULTIPLIED BY THE RESISTANCE FACTOR OF 0.5 USING MODIFIED GATES TO DETERMINE DRIVEN PILE CAPACITY.

**LIST OF DRAWINGS**

1. GENERAL PLAN
2. CROSS SECTION & QUANTITIES
3. SOUTH ABUTMENT
4. SOUTH ABUTMENT DETAILS
5. SOUTH ABUTMENT
6. NORTH ABUTMENT
7. NORTH ABUTMENT DETAILS
8. 45W PRESTRESSED ORDER DETAILS 1
9. 45W PRESTRESSED ORDER DETAILS 2
10. STEEL DIAPHRAGM CROSS SECTION
11. SUPERSTRUCTURE PLAN
12. SUPERSTRUCTURE DETAILS
13. APPROACH SLAB
14. APPROACH SLAB DETAILS
15. SINGLE SLOPE PARAPET 3255
16. MODIFIED SLOPE-PARAPET 3255

**TRAFFIC VOLUME**

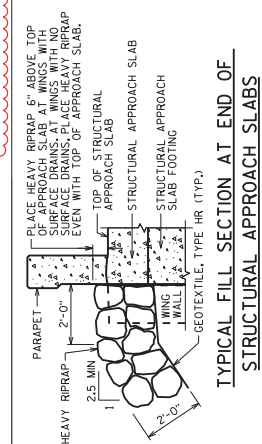
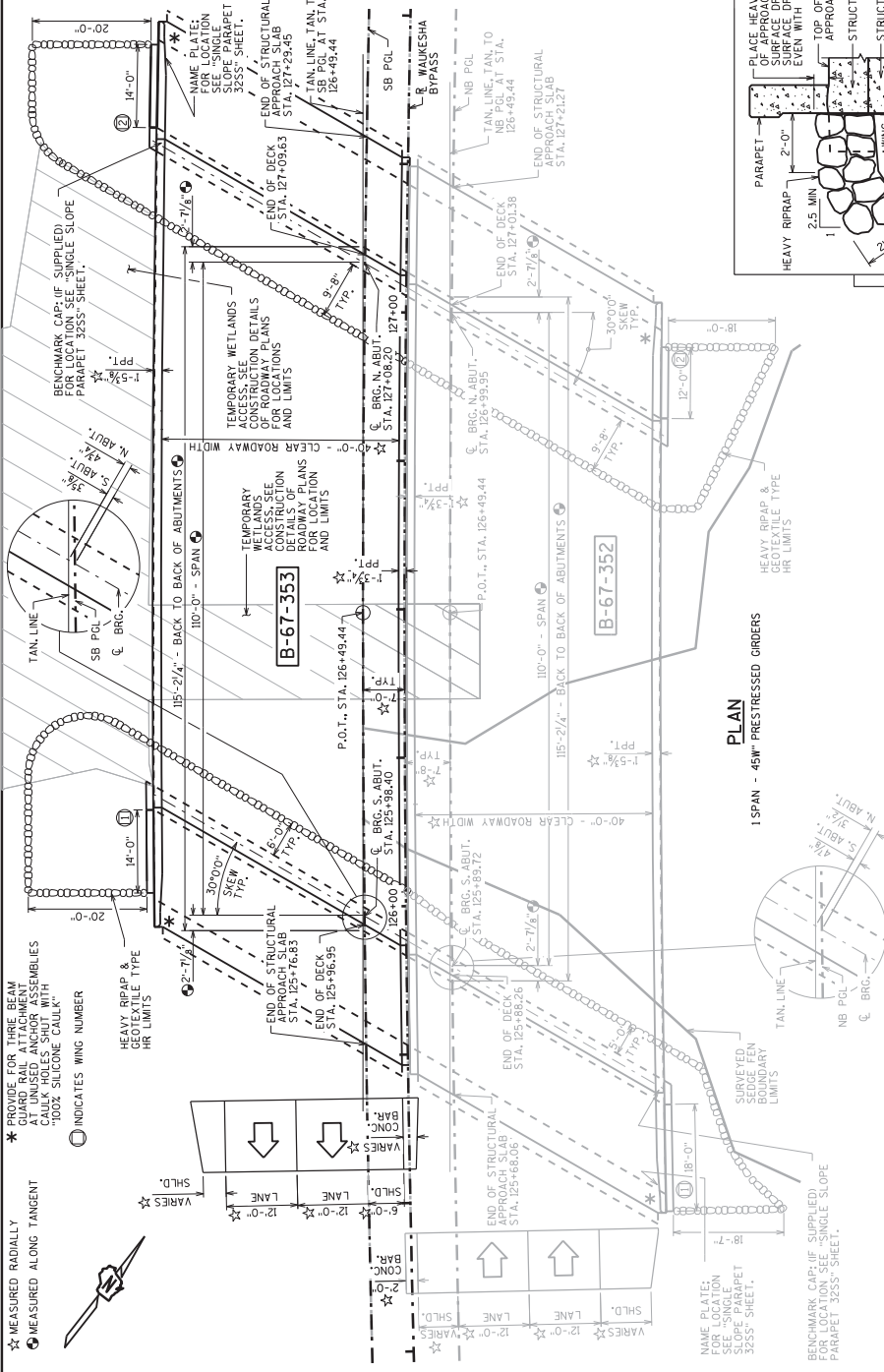
WALKESHA BYPASS  
ADT = 17,300 (2037)  
R.D.S. = 50 MP.H.

**CURVE DATA**

WALKESHA BYPASS  
P.I. = 125+62.72  
 $\Delta = 274.849^\circ$   
T = 940.45  
L = 1897.77  
R = 5000  
S.E. = 2.0 %  
P.C. = 105+02.27  
P.T. = 135+00.04

*William C. Decker*  
6/25/18

|                                                            |                     |          |
|------------------------------------------------------------|---------------------|----------|
| ADD ELECTRICAL DETAIL                                      | MJK                 | BY       |
| NO. DATE                                                   | REVISION            |          |
| <b>BUREAU OF STRUCTURES</b>                                |                     |          |
| <i>William C. Decker</i>                                   |                     |          |
| ACCEPTED <b>11/11/17</b>                                   |                     |          |
| CHIEF STRUCTURES DESIGN ENGINEER DATE                      |                     |          |
| <b>STRUCTURE B-67-353</b>                                  |                     |          |
| COUNTY                                                     | TOWN                | WALKESHA |
| DESIGN SPEC. WASHINGTON STATE BRIDGE DESIGN SPECIFICATIONS |                     |          |
| BY DRAWN                                                   | SEM. BR. MJK (C.D.) | PLANS    |
| BY CHECKED                                                 | SEM. BR. MJK (C.D.) | DMB      |
| GENERAL PLAN                                               |                     |          |
| SHEET 1 OF 19                                              |                     |          |
| 1280                                                       |                     |          |



Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1280  
July 2, 2018

\* MEASURED RADIALLY  
☆ MEASURED ALONG TANGENT  
○ INDICATES WING NUMBER

PROVIDE FOR THREE BEAM GUARD RAIL ATTACHMENT AT UNUSED ANCHOR ASSEMBLIES 100% SILICONE CAULKING

VARIES RADially

VARIES ALONG TANGENT

VARIES

VARIES

VARIES

VARIES

VARIES

VARIES

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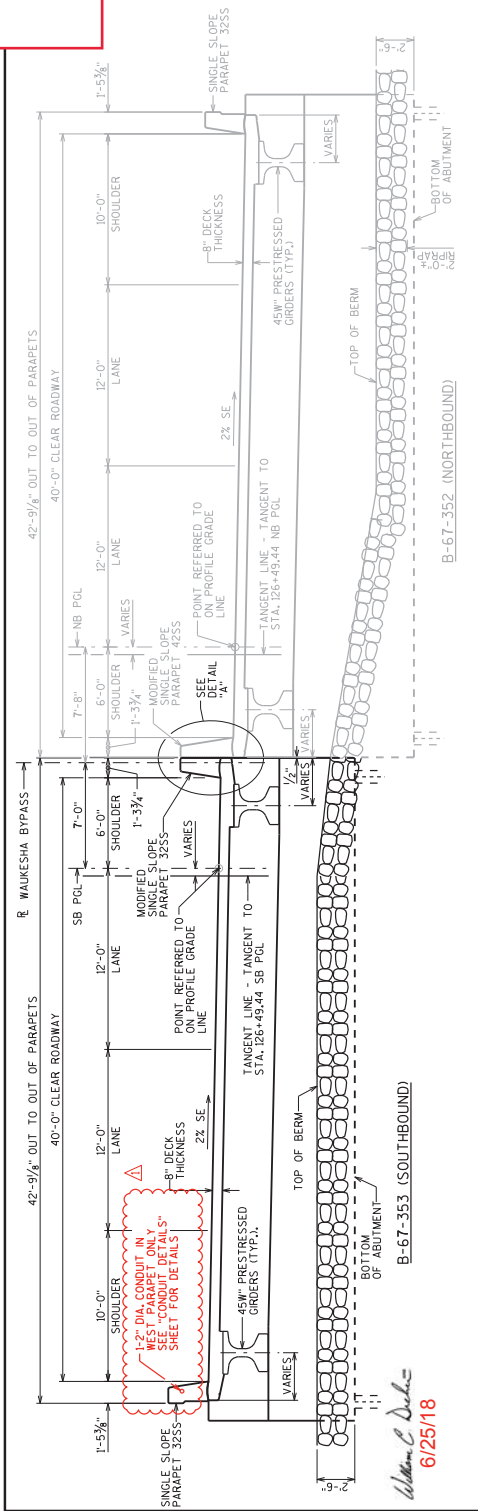
VARIES

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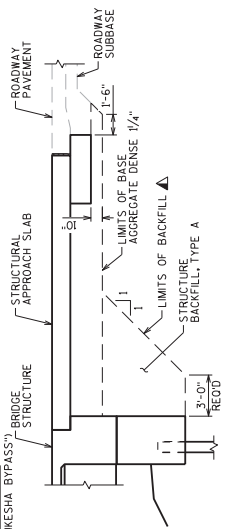
VARIES

STATE PROJECT NUMBER  
**2788-00-71**

**Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1281  
July 2, 2018**

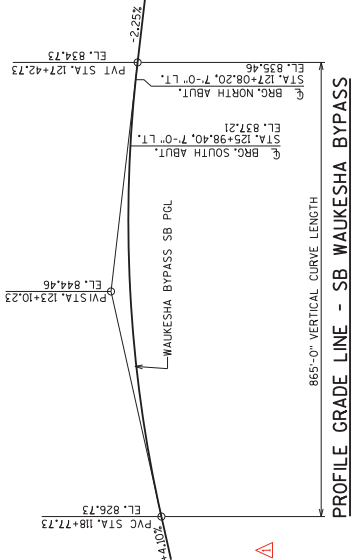


**CROSS SECTION THRU ROADWAY LOOKING NORTH (UPSTATION)**  
(ALL HORIZONTAL DIMENSIONS ARE MEASURED RADially - NORMAL TO THE CURVED - WALKESHA BYPASS)



**TYPICAL SECTION THRU ABUTMENT**

(1) ABUTMENT WITH STRUCTURAL APPROACH  
BACKFILL PAY LIMITS BEYOND BACKFILL PAY LIMITS SHALL BE INCIDENTAL TO EXCAVATION FOR STRUCTURES. LIMITS OF EXCAVATION SHALL BE DETERMINED BY THE CONTRACTOR.



**GENERAL NOTES**

- DRAWINGS SHALL NOT BE SCALED.
- BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2" CLEAR UNLESS OTHERWISE SHOWN OR NOTED.
- THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE.
- AT THE BACK FACE OF ABUTMENT ALL VOLUME WHICH IS OCCUPIED BY THE NEW STRUCTURE SHALL BE BACKFILLED WITH STRUCTURE BACKFILL. ALSO AS DETAILLED ON THE STRUCTURAL APPROACH SLAB SHEETS.
- THE QUANTITY FOR BACKFILL STRUCTURE IS CALCULATED BASED ON THE DETAIL SHOWN IN THE PLANS.
- ELASTOMERIC BEARING PAIDS NEED NOT BE INDIVIDUALLY MOUNTED PROVIDED THE CUT EDGES ARE SMOOTH AND TRUE.
- PROTECTIVE SURFACE TREATMENT TO BE APPLIED TO THE ENTIRE EXPOSED TOP OF DECK & APPROACH SLAB SURFACES AND TO THE VERTICAL AND HORIZONTAL SURFACES OF THE PAVING NOTICES AT ABUTMENT DIAPHRAGMS.
- PROTECTIVE SURFACE SEALER TO BE APPLIED TO THE FRONT FACE AND THE TOP OF PARAPETS, INCLUDING PARAPETS ON APPROACH SLABS.
- THE SLOPE OF THE FILL IN FRONT OF THE ABUTMENTS SHALL BE COVERED WITH HEAVY RIPRAP AND "GEOTEXTILE TYPE HR" TO THE EXTENT SHOWN ON SHEET 1 AND IN THE ABUTMENT DETAILS.
- BEVEL EXPOSED EDGES OF CONCRETE  $\frac{3}{4}$ " UNLESS OTHERWISE NOTED.
- THE HAUNCH CONCRETE QUANTITY IS BASED ON THE AVERAGE HAUNCH SHOWN ON THE "GRID DETAILS 2" SHEET 1.

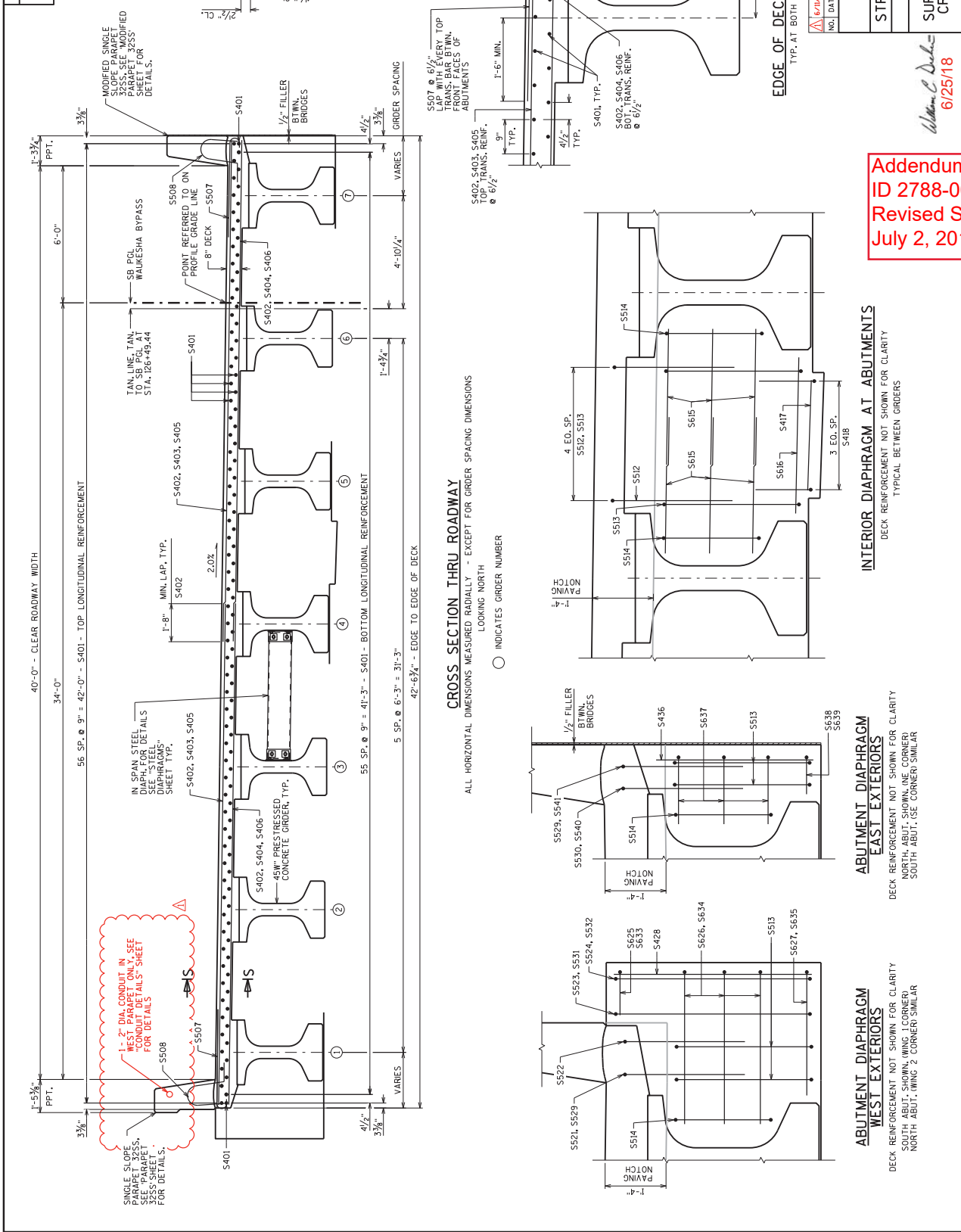
**TOTAL ESTIMATED QUANTITIES**

| BID ITEM NUMBER | BID ITEMS                                       | UNIT | SUPER. | SOUTH APPROACH SLAB | SOUTH ABUT. | NORTH ABUT. | NORTH APPROACH SLAB | TOTALS             |
|-----------------|-------------------------------------------------|------|--------|---------------------|-------------|-------------|---------------------|--------------------|
| 206.1000        | EXCAVATION FOR STRUCTURES BRIDGES B-67-353      | LS   |        |                     |             |             |                     | 1                  |
| 210.1500        | BACKFILL STRUCTURE TYPE A                       | TON  |        | 180                 | 180         |             |                     | 340                |
| 305.0020        | BASE AGGREGATE DENSE 1 1/4-INCH                 | TON  |        | 130                 | 128         |             |                     | 258                |
| 501.000.05      | ICE HOT WEATHER CONCRETING                      | LB   | 1,605  | 460                 | 325         | 450         |                     | 3,170              |
| 502.0100        | CONCRETE MASONRY BRIDGES                        | CY   | 214    | 61                  | 44          | 60          |                     | 422                |
| 502.3200        | PROTECTIVE SURFACE TREATMENT                    | SY   | 524    | 90                  |             | 89          |                     | 703                |
| 502.3210        | PIGMENTED SURFACE SEALER                        | SY   | 92     | 17                  |             | 17          |                     | 126                |
| 505.0040        | PRESTRESSED ORDER TYPE I 45W-INCH               | LF   | 777    |                     |             |             |                     | 777                |
| 505.0400        | BAR STEEL REINFORCEMENT HS STRUCTURES           | LB   |        | 3,275               | 2,995       |             |                     | 6,270              |
| 505.0600        | BAR STEEL REINFORCEMENT HS COATED STRUCTURES    | LB   | 31840  | 10,870              | 800         | 785         | 10,870              | 55,165             |
| 505.0800.05     | BAR STEEL REINFORCEMENT HS STAINLESS STRUCTURES | LB   | 1,465  |                     |             |             |                     | 1,465              |
| 506.2805        | BEARING PAIDS ELASTOMERIC NON-LIMITED           | EACH | 14     |                     |             |             |                     | 14                 |
| 506.4000        | STEEL DIAPHRAGMS B-67-353                       | SY   | 12     |                     |             |             |                     | 12                 |
| 516.0500        | RUBBERIZED MEMBRANE WATERPROOFING               | SY   |        | 11                  | 11          |             |                     | 22                 |
| 550.0500        | PILE POINTS                                     | EACH |        | 10                  | 10          |             |                     | 20                 |
| 550.1120        | PILENG STEEL HP 12-INCH X 53 LB                 | LF   |        | 600                 | 850         |             |                     | 1,450              |
| 606.0300        | RRRAP HEAVY                                     | CY   |        | 75                  | 85          |             |                     | 160                |
| 612.0406        | PIPE UNDERDRAIN WRAPPED 6-INCH                  | LF   | 2      |                     |             |             |                     | 2                  |
| 614.0150        | ANCHOR ASSEMBLIES FOR STEEL PLATE BEAM GUARD    | EACH |        | 80                  | 80          |             |                     | 160                |
| 625.0120        | CONDUIT RIGID METALLIC 2-INCH                   | LF   |        | 115                 | 123         |             |                     | 238                |
| 632.0125        | CONDUIT RIGID METALLIC SCHEDULE 40 2-INCH       | LF   |        | 10                  |             |             |                     | 10                 |
| 632.0225        | CONDUIT RIGID NONMETALLIC SCHEDULE 40 2-INCH    | LF   |        | 115                 | 23          |             |                     | 138                |
| 653.0220        | JUNCTION BOXES 18X6X6-INCH                      | EACH | 2      |                     |             |             |                     | 2                  |
|                 | NON-BID ITEMS                                   |      |        |                     |             |             |                     |                    |
|                 | FILLER                                          | SEE  |        |                     |             |             |                     | 1/2" - 3/4" - 1/2" |

**CROSS SECTION & QUANTITIES**  
SHEET 2  
**1281**



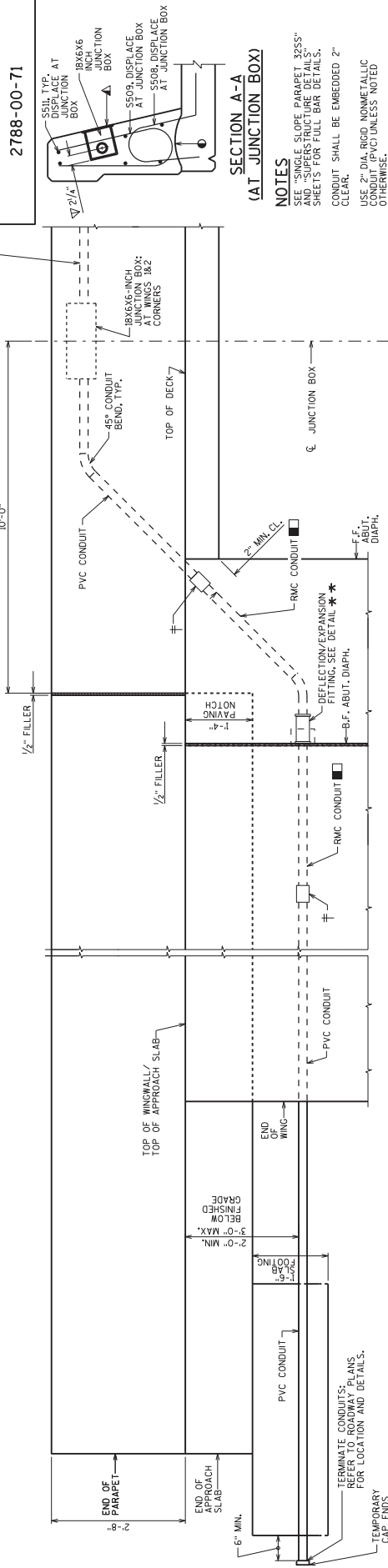
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|-------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-----------------------|----------|-----------------------|----------|-----|----|---------------------------------------------------------------------------------|--|--|--|--|---------------------------|--|--|--|--|------------------------------|--|--|--|--|----------|--|--|--|--|------|--|--|--|--|
| STATE PROJECT NUMBER<br><b>2788-00-71</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                             | <b>8</b>              |          |                       |          |     |    |                                                                                 |  |  |  |  |                           |  |  |  |  |                              |  |  |  |  |          |  |  |  |  |      |  |  |  |  |
| <table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td style="width: 20%; padding: 2px;">NO. DATE</td> <td style="width: 20%; padding: 2px;">ADD ELECTRICAL DETAIL</td> <td style="width: 20%; padding: 2px;">REVISION</td> <td style="width: 20%; padding: 2px;">MJK</td> <td style="width: 20%; padding: 2px;">BY</td> </tr> <tr> <td colspan="5" style="padding: 2px; text-align: center;">STATE OF WISCONSIN<br/>DEPARTMENT OF TRANSPORTATION<br/>STRUCTURES DESIGN SECTION</td> </tr> <tr> <td colspan="5" style="padding: 2px; text-align: center;">STRUCTURE <b>B-67-353</b></td> </tr> <tr> <td colspan="5" style="padding: 2px; text-align: center;">SUPERSTRUCTURE CROSS SECTION</td> </tr> <tr> <td colspan="5" style="padding: 2px; text-align: center;">SHEET 11</td> </tr> <tr> <td colspan="5" style="padding: 2px; text-align: center;">1290</td> </tr> </table> |                       | NO. DATE | ADD ELECTRICAL DETAIL | REVISION | MJK | BY | STATE OF WISCONSIN<br>DEPARTMENT OF TRANSPORTATION<br>STRUCTURES DESIGN SECTION |  |  |  |  | STRUCTURE <b>B-67-353</b> |  |  |  |  | SUPERSTRUCTURE CROSS SECTION |  |  |  |  | SHEET 11 |  |  |  |  | 1290 |  |  |  |  |
| NO. DATE                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                              | ADD ELECTRICAL DETAIL | REVISION | MJK                   | BY       |     |    |                                                                                 |  |  |  |  |                           |  |  |  |  |                              |  |  |  |  |          |  |  |  |  |      |  |  |  |  |
| STATE OF WISCONSIN<br>DEPARTMENT OF TRANSPORTATION<br>STRUCTURES DESIGN SECTION                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                       |                       |          |                       |          |     |    |                                                                                 |  |  |  |  |                           |  |  |  |  |                              |  |  |  |  |          |  |  |  |  |      |  |  |  |  |
| STRUCTURE <b>B-67-353</b>                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                             |                       |          |                       |          |     |    |                                                                                 |  |  |  |  |                           |  |  |  |  |                              |  |  |  |  |          |  |  |  |  |      |  |  |  |  |
| SUPERSTRUCTURE CROSS SECTION                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                          |                       |          |                       |          |     |    |                                                                                 |  |  |  |  |                           |  |  |  |  |                              |  |  |  |  |          |  |  |  |  |      |  |  |  |  |
| SHEET 11                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                              |                       |          |                       |          |     |    |                                                                                 |  |  |  |  |                           |  |  |  |  |                              |  |  |  |  |          |  |  |  |  |      |  |  |  |  |
| 1290                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                  |                       |          |                       |          |     |    |                                                                                 |  |  |  |  |                           |  |  |  |  |                              |  |  |  |  |          |  |  |  |  |      |  |  |  |  |



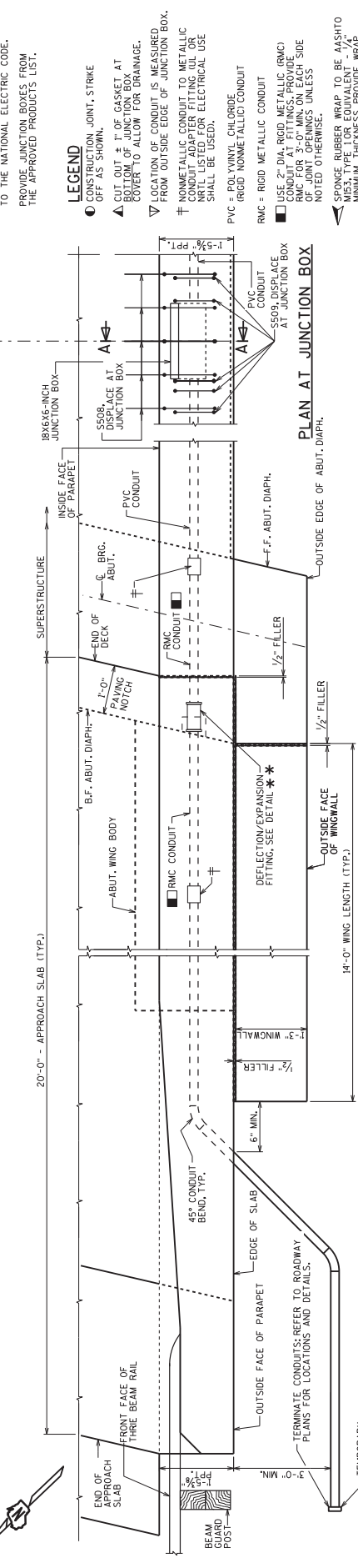
Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1290  
July 2, 2018

*William C. Decker*  
6/25/18

STATE PROJECT NUMBER  
**2788-00-71**



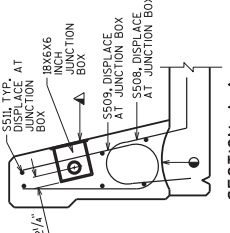
**OUTSIDE ELEVATION VIEW OF WINGWALL, STRUCTURAL APPROACH SLAB & PARAPET SHOWING CONDUITS**  
(WING 2 SHOWN, WING 1 SIMILAR)



**PLAN VIEW OF WINGWALL, STRUCTURAL APPROACH SLAB & PARAPET SHOWING CONDUITS**  
(WING 2 SHOWN, WING 1 SIMILAR)



**DEFLECTION/EXPANSION FITTING DETAIL**  
BOND JUMPER NOT SHOWN FOR CLARITY



**SECTION A-A (AT JUNCTION BOX)**

- NOTES**
- SEE "SLOPE PARAPET, 3955" AND "SUPERSTRUCTURE DETAILS" SHEETS FOR FULL BAR DETAILS.
  - CONDUIT SHALL BE EMBEDDED 2" CLEAR.
  - USE 2" DIA. RIGID NONMETALLIC CONDUIT (PVC) UNLESS NOTED OTHERWISE.
  - CONDUIT FITTINGS, CONDUIT BENDS, AND ADAPTER FITTINGS INCIDENTAL TO CONDUIT WORK.
  - CONDUIT BENDS SHALL CONFORM TO THE NATIONAL ELECTRIC CODE.
  - PROVIDE JUNCTION BOXES FROM THE APPROVED PRODUCTS LIST.

- LEGEND**
- CONSTRUCTION JOINT, STRIKE OFF AS SHOWN.
  - ▲ CUT OUT ± 1" OF GASKET AT BOTTOM OF JUNCTION BOX COVER TO ALLOW FOR DRAINAGE.
  - ▽ LOCATION OF CONDUIT IS MEASURED FROM OUTSIDE EDGE OF JUNCTION BOX.
  - ≠ NONMETALLIC CONDUIT TO METALLIC CONDUIT ADAPTER FITTING UL OR SHALL BE USED FOR ELECTRICAL USE SHALL BE USED.
  - PVC = POLYVINYL CHLORIDE RIGID NONMETALLIC CONDUIT
  - RMC = RIGID METALLIC CONDUIT
  - USE 2" DIA. RIGID METALLIC (RMC) CONDUIT AT FITTINGS, PROVIDE RMC FOR 3'-0" MIN. ON EACH SIDE OF JOINT OPENINGS UNLESS NOTED OTHERWISE.
  - ◀ SPONGE RUBBER WRAP TO BE AASHTO M 288.5 TYPE I. PROVIDE MINIMUM THICKNESS. PROVIDE WRAP FOR THE ENTIRE LENGTH OF THE FITTING OR AS SHOWN. SPONGE FITTING OR AS SHOWN. SPONGE FITTING OR AS SHOWN. SPONGE FITTING OR AS SHOWN.
  - ◀ CONDUIT RIGID METALLIC 2-INCH.

\*\* DEFLECTION/EXPANSION FITTING REQUIREMENTS: UP TO 3" CONDUIT CONTRACTION OR EXPANSION AND UP TO 30 DEGREES OF EXPANSION AND UP TO 30 DEGREES OF CONTRACTION IN ANY DIRECTION WITH BONDING JUMPER

**Addendum No. 01**  
**ID 2788-00-71**  
**Added Sheet 1297A**  
**July 2, 2018**

*William C. Decker*  
**6/25/18**

CONDUIT DETAILS  
SHEET 19  
**1297A**

| NO.     | DATE | REVISION              | MLK |
|---------|------|-----------------------|-----|
| 6/14/18 |      | ADD ELECTRICAL DETAIL | MLK |

|                                                                                 |
|---------------------------------------------------------------------------------|
| STATE OF WISCONSIN<br>DEPARTMENT OF TRANSPORTATION<br>STRUCTURES DESIGN SECTION |
| <b>STRUCTURE B-67-353</b>                                                       |
| DESIGNED BY: [ ]<br>CHECKED BY: [ ]                                             |

STATE PROJECT NUMBER  
**2788-00-71**

DESIGN DATA

LIVE LOADS:  
DESIGN LOADS: HL-93  
INVENTORY RATING FACTOR: RF=1.00  
OPERATING RATING FACTOR: RF=1.51  
WISCONSIN STANDARD PERMIT VEHICLE (WIS-SPV): 250 (KIPS)  
STRUCTURE IS DESIGNED FOR A FUTURE WEARING SURFACE OF 20 POUNDS PER SQUARE FOOT.

MATERIAL PROPERTIES:  
CONCRETE MASONRY: FC = 4000 P.S.I.  
SUPERSTRUCTURE: FC = 3500 P.S.I.  
ALL OTHER: FC = 3500 P.S.I.

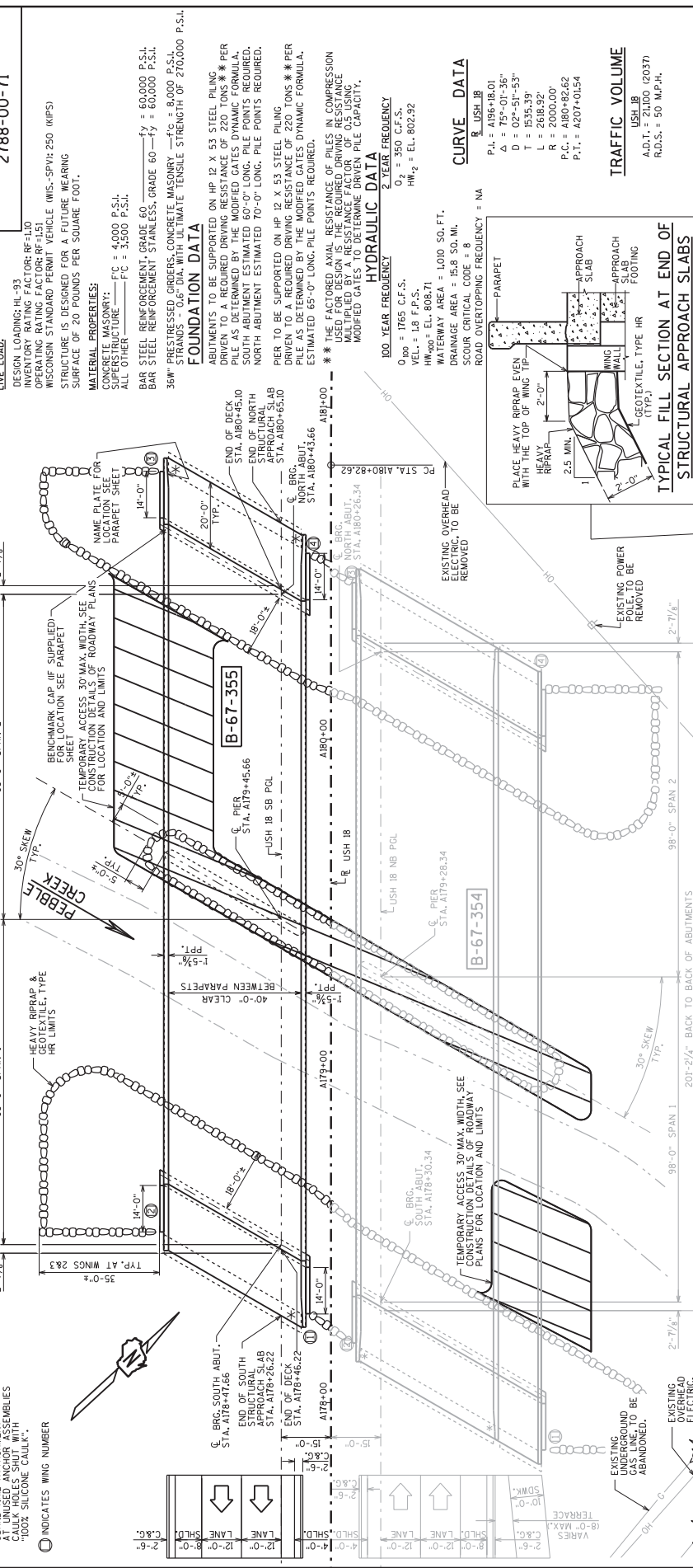
BAR STEEL REINFORCEMENT, GRADE 60  
BAR STEEL REINFORCEMENT, STAINLESS, GRADE 60 — fy = 60,000 P.S.I.  
STRANDS - 0.6" DIA. WITH ULTIMATE TENSILE STRENGTH OF 270,000 P.S.I.

**FOUNDATION DATA**  
ABUTMENTS TO BE SUPPORTED ON HP 12 X 53 STEEL PILING DRIVEN TO A REQUIRED DRIVING RESISTANCE OF 220 TONS \*\* PER PILE AS DETERMINED BY THE MODIFIED GATES DYNAMIC FORMULA. NORTH ABUTMENT ESTIMATED 60'-0" LONG. PILE POINTS REQUIRED, 12 PILES. SOUTH ABUTMENT ESTIMATED 70'-0" LONG. PILE POINTS REQUIRED, 12 PILES. PIER TO BE SUPPORTED ON HP 12 X 53 STEEL PILING DRIVEN TO A REQUIRED DRIVING RESISTANCE OF 220 TONS \*\* PER PILE AS DETERMINED BY THE MODIFIED GATES DYNAMIC FORMULA. ESTIMATED 65'-0" LONG. PILE POINTS REQUIRED, 12 PILES. \*\* THE FACTORED AXIAL RESISTANCE OF PILES IN COMPRESSION USED FOR DESIGN IS THE REQUIRED DRIVING RESISTANCE MODIFIED GATES TO DETERMINE DRIVEN PILE CAPACITY.

**HYDRAULIC DATA**  
100-YEAR FREQUENCY:  
Q = 1765 C.F.S.  
V = 1.8 F.P.S.  
HW<sub>100</sub> = EL. 808.71  
DRAINAGE AREA = 15.8 SQ. MI.  
SCOUR CRITICAL CODE = 8  
ROAD OVERTOPPING FREQUENCY = NA

**CURVE DATA**  
R = 1878.18 FT.  
P.L. = 1878.18 FT.  
D = 75'-01"-36"  
T = 1555.39'  
L = 2618.92'  
R = 20000.00'  
P.C. = A180+82.62  
P.T. = A207+01.54

**TRAFFIC VOLUME**  
USH 18  
A.D.T. = 21,100 (2037)  
R.D.S. = 50 M.P.H.



**LIST OF DRAWINGS**

- GENERAL PLAN
- GENERAL SECTION & QUANTITIES
- SUBSURFACE EXPLANATION
- SOUTH ABUTMENT
- SOUTH ABUTMENT DETAILS
- NORTH ABUTMENT
- NORTH ABUTMENT DETAILS
- PIER
- PIER
- 36" PRESTRESSED ORDER DETAILS 1
- 36" PRESTRESSED ORDER DETAILS 2
- STEEL DIAPHRAGM
- SUPERSTRUCTURE
- SUPERSTRUCTURE DIAPH. DETAILS
- SUPERSTRUCTURE DETAILS
- SOUTH APPROACH SLAB DETAILS
- NORTH APPROACH SLAB DETAILS
- SINGLE SLOPE PARAPET 325
- SINGLE SLOPE PARAPET 325

**REVISIONS**

| NO. | DATE    | REVISION              | BY  |
|-----|---------|-----------------------|-----|
| 1   | 6/11/18 | ADD ELECTRICAL DETAIL | MJK |

STRUCTURE DESIGN CONTACTS:  
DOMINIQUE BECHLE (608) 261-8205  
AARON BOWK (608) 261-0261

**BUREAU OF STRUCTURES**  
11/11/17  
ACCEPTED: *William C. Decker*  
CHIEF STRUCTURES DESIGN ENGINEER DATE

STRUCTURE B-67-355  
USH 18 SOUTHBOUND OVER PEBBLE CREEK  
COUNTY: WALKESHA TOWN: WALKESHA  
DESIGN SPEC: WISCONSIN STANDARD PERMIT VEHICLE (WIS-SPV): 250 (KIPS)  
ASHTO LRFD BRIDGE DESIGN SPECIFICATIONS  
DESIGNED BY: MJK (B) / MBS / JPH (C/D) / DMB  
DRAWN BY: MJK (B) / MBS / JPH (C/D) / DMB

GENERAL PLAN  
SHEET 1 OF 19  
1320

DATE: JUNE 2017  
I.D.: 2788-00-0F

**ADDENDUM NO. 01**  
ID 2788-00-71  
Revised Sheet 1320  
July 2, 2018

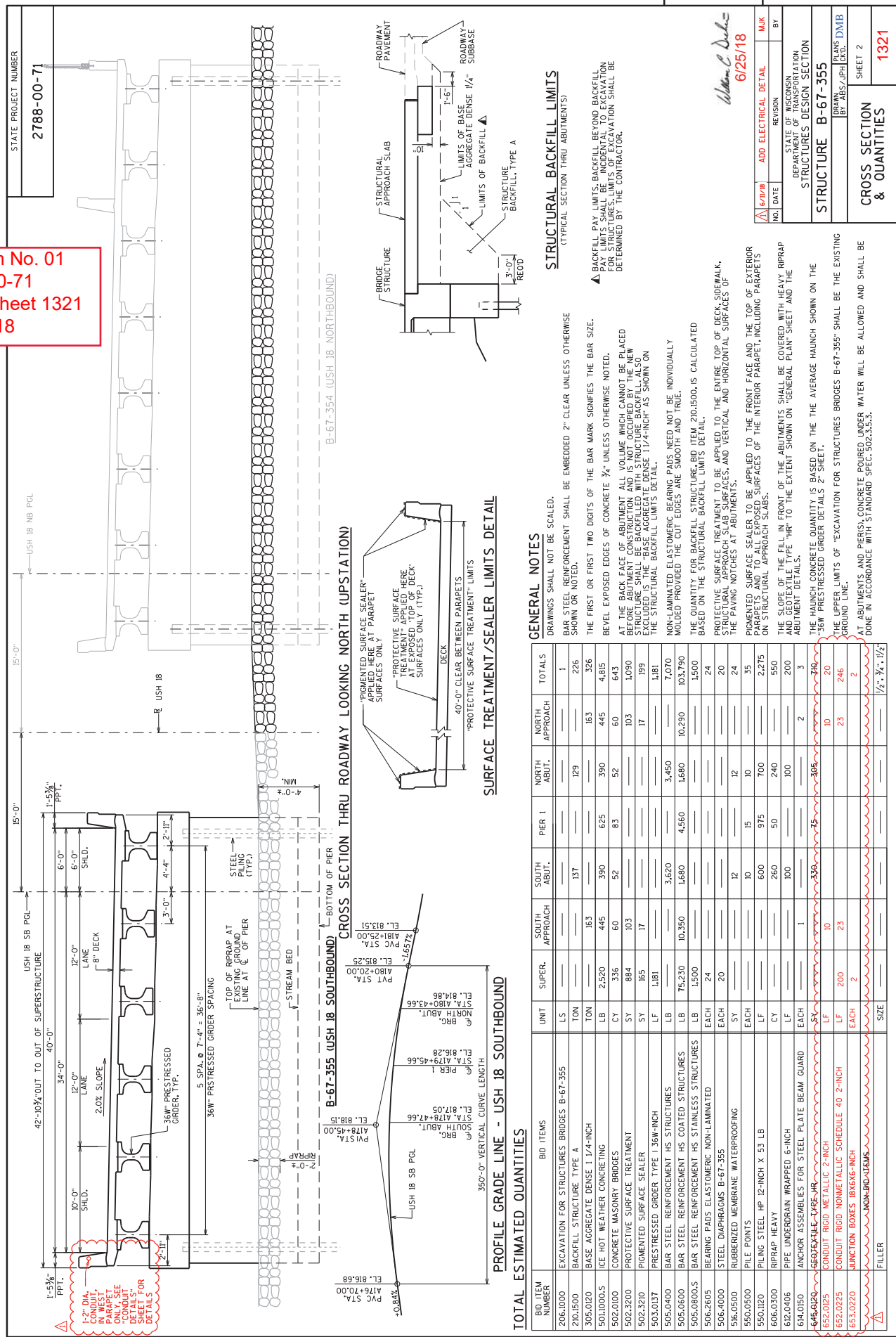
*William C. Decker*  
6/25/18

100-YEAR FREQUENCY:  
Q = 1765 C.F.S.  
V = 1.8 F.P.S.  
HW<sub>100</sub> = EL. 808.71  
DRAINAGE AREA = 15.8 SQ. MI.  
SCOUR CRITICAL CODE = 8  
ROAD OVERTOPPING FREQUENCY = NA

**CURVE DATA**  
R = 1878.18 FT.  
P.L. = 1878.18 FT.  
D = 75'-01"-36"  
T = 1555.39'  
L = 2618.92'  
R = 20000.00'  
P.C. = A180+82.62  
P.T. = A207+01.54

**TRAFFIC VOLUME**  
USH 18  
A.D.T. = 21,100 (2037)  
R.D.S. = 50 M.P.H.

Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1321  
 July 2, 2018



STATE PROJECT NUMBER  
 2788-00-71

**STRUCTURAL BACKFILL LIMITS**  
 (TYPICAL SECTION THRU ABUTMENTS)

▲ BACKFILL PAY LIMITS: BACKFILL BEYOND BACKFILL PAY LIMITS SHALL BE INCIDENTAL TO EXCAVATION DETERMINED BY THE CONTRACTOR.

**GENERAL NOTES**

- DRAWINGS SHALL NOT BE SCALED.
- BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2' CLEAR UNLESS OTHERWISE SHOWN OR NOTED.
- THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE.
- BEVEL EXPOSED EDGES OF CONCRETE 3/4" UNLESS OTHERWISE NOTED.
- AT THE BACK FACE OF ABUTMENT ALL VOLUME WHICH CANNOT BE PLACED BEFORE ABUTMENT CONSTRUCTION AND IS NOT OCCUPIED BY THE NEW STRUCTURE SHALL BE BACKFILLED WITH STRUCTURE BACKFILL. ALSO THE STRUCTURE SHALL BE BACKFILLED WITH STRUCTURE BACKFILL TO THE STRUCTURAL BACKFILL LIMITS DETAIL.
- NON-LAMINATED ELASTOMERIC BEARING PADS NEED NOT BE INDIVIDUALLY MOLDED PROVIDED THE CUT EDGES ARE SMOOTH AND TRUE.
- THE QUANTITY FOR BACKFILL STRUCTURE, BD ITEM 210:1500.15 CALCULATED BASED ON THE STRUCTURAL BACKFILL LIMITS DETAIL.
- PROTECTIVE SURFACE TREATMENT TO BE APPLIED TO THE ENTIRE TOP OF DECK, SIDEWALK, STRUCTURAL APPROACH SLABS, CURBS, PARAPETS, AND VERTICAL AND HORIZONTAL SURFACES OF THE PILING NOTICES AT ABUTMENTS.
- PONTOON SURFACE SEALER TO BE APPLIED TO THE FRONT FACE AND THE TOP OF EXTERIOR SURFACES OF THE SURFACES OF THE INTERIOR PARAPET, INCLUDING PARAPETS ON STRUCTURAL APPROACH SLABS.
- THE SLOPE OF THE FILL IN FRONT OF THE ABUTMENTS SHALL BE COVERED WITH HEAVY RIPRAP AND GEOTEXTILE TYPE "HR" TO THE EXTENT SHOWN ON "GENERAL PLAN" SHEET AND THE ABUTMENT DETAILS.
- 36W PRESTRESSED GIRDER DETAILS 2' SHEET.
- THE HANCHO CONCRETE QUANTITY IS BASED ON THE AVERAGE HANCHO SHOWN ON THE GROUND LINE.
- THE UPPER LIMITS OF "EXCAVATION FOR STRUCTURES BRIDGES B-67-355" SHALL BE THE EXISTING GROUND LINE.
- ABUTMENTS AND PIERS: CONCRETE SOAKED UNDER WATER WILL BE ALLOWED AND SHALL BE DONE IN ACCORDANCE WITH STANDARD SPEC 502.3.3.3.3.

**SURFACE TREATMENT/SEALER LIMITS DETAIL**

**TOTAL ESTIMATED QUANTITIES**

| BID ITEM NUMBER | BID ITEMS                                       | UNIT | SUPER. | SOUTH APPROACH | PIER 1 | NORTH ABUT. | NORTH APPROACH | TOTALS           |
|-----------------|-------------------------------------------------|------|--------|----------------|--------|-------------|----------------|------------------|
| 206:1000        | EXCAVATION FOR STRUCTURES BRIDGES B-67-355      | LS   |        |                |        |             |                | 1                |
| 210:1500        | BACKFILL STRUCTURE TYPE A                       | TON  |        | 137            |        | 129         |                | 226              |
| 305:020         | BASE AGGREGATE DENSE 1 1/4-INCH                 | TON  |        | 163            |        | 390         | 445            | 326              |
| 501:000S        | ICE HOT WEATHER CONCRETING                      | LB   | 2,520  | 445            | 625    | 52          | 643            | 4,815            |
| 502:0100        | CONCRETE MASONRY BRIDGES                        | CY   | 336    | 60             | 83     |             |                | 60               |
| 502:3200        | PROTECTIVE SURFACE TREATMENT                    | SY   | 884    | 103            |        | 103         |                | 1,090            |
| 502:3210        | PIGMENTED SURFACE SEALER                        | SY   | 165    | 17             |        |             |                | 17               |
| 503:0137        | PRESTRESSED GIRDER TYPE 1 36W-INCH              | LF   | 1,181  |                |        |             |                | 1,181            |
| 505:0400        | BAR STEEL REINFORCEMENT HS STRUCTURES           | LB   |        | 3,620          |        | 3,450       |                | 7,070            |
| 505:0600        | BAR STEEL REINFORCEMENT HS COATED STRUCTURES    | LB   | 75,230 | 10,350         | 4,560  | 1,680       | 10,290         | 103,790          |
| 505:0800S       | BAR STEEL REINFORCEMENT HS STAINLESS STRUCTURES | LB   | 1,500  |                |        |             |                | 1,500            |
| 506:2605        | BEARING PADS ELASTOMERIC NON-LAMINATED          | EACH | 24     |                |        |             |                | 24               |
| 506:4000        | STEEL DIAPHRAGMS B-67-355                       | EACH | 20     |                |        |             |                | 20               |
| 516:0500        | RUBBERIZED MEMBRANE WATERPROOFING               | SY   |        | 12             |        | 12          |                | 24               |
| 550:0500        | PILE POINTS                                     | EACH |        | 10             | 15     | 10          |                | 35               |
| 550:1120        | PILING STEEL HP 12-INCH X 53 LB                 | LF   |        | 600            | 975    | 700         |                | 2,275            |
| 606:0300        | RRAP HEAVY                                      | CY   |        | 260            | 50     | 240         |                | 550              |
| 612:0406        | PIPE UNDERDRAIN WRAPPED 6-INCH                  | LF   |        | 100            |        | 100         |                | 200              |
| 614:0150        | ANCHOR ASSEMBLIES FOR STEEL PLATE BEAM GUARD    | EACH |        | 1              |        |             | 2              | 3                |
| 646:0220        | GEOTEXTILE TYPE "HR"                            | SY   |        | 330            | 75     | 306         |                | 710              |
| 652:0225        | CONDUIT RIGID METALLIC 2-INCH                   | LF   |        | 10             |        | 10          |                | 20               |
| 652:0225        | CONDUIT RIGID NONMETALLIC SCHEDULE 40 2-INCH    | LF   |        | 200            | 23     | 246         |                | 2                |
| 653:0220        | JUNCTION BOXES 18X6X6-INCH                      | EACH |        | 2              |        | 2           |                | 2                |
|                 | NON-BID ITEMS                                   |      |        |                |        |             |                |                  |
|                 | FILLER                                          | SIZE |        |                |        |             |                | 1/2", 3/4", 1/2" |

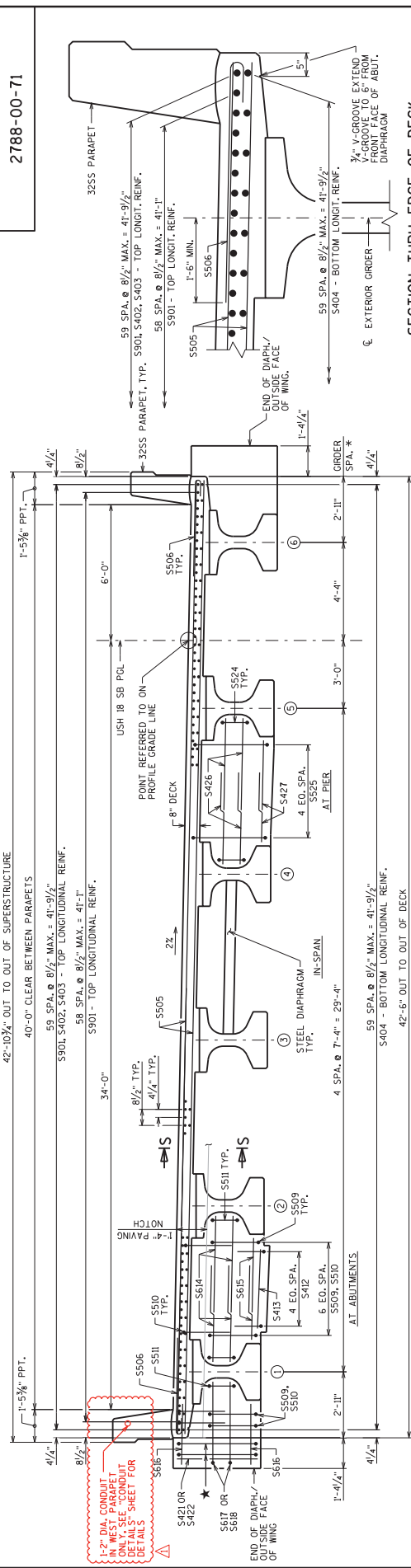
Walter C. Deane  
 6/25/18

**CROSS SECTION & QUANTITIES**  
 SHEET 2  
 1321

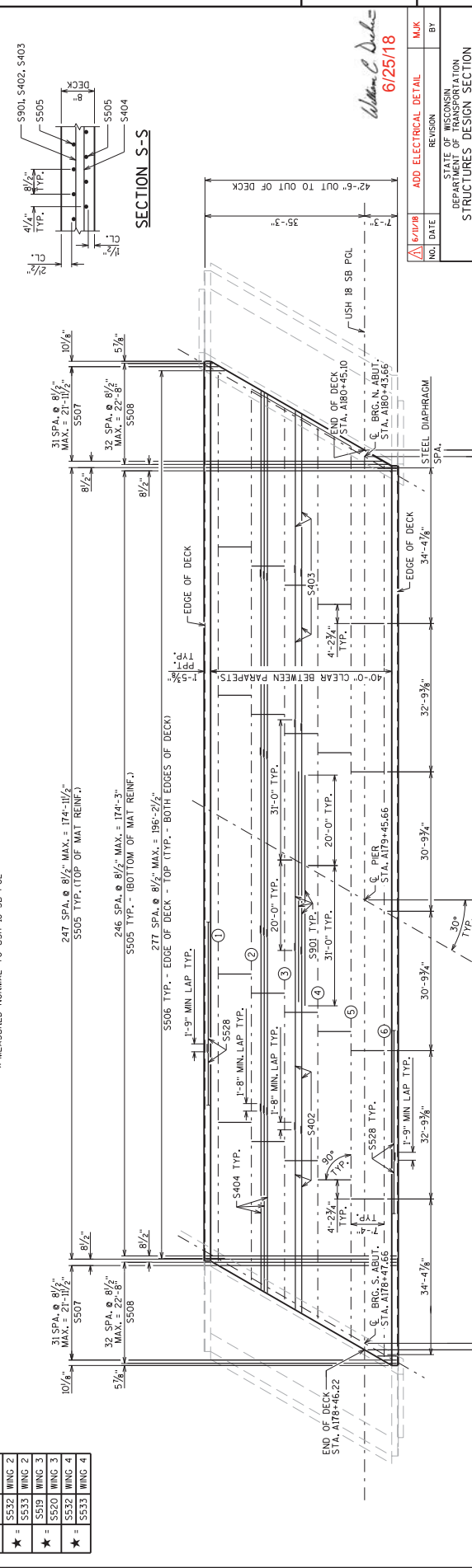
ADD ELECTRICAL DETAIL  
 NO. DATE REVISION BY

STATE OF WISCONSIN  
 DEPARTMENT OF TRANSPORTATION  
 STRUCTURES DESIGN SECTION  
 STRUCTURE B-67-355  
 BRN/ABS/JPH/CKD/DMB  
 PLANS

STATE PROJECT NUMBER  
**2788-00-71**



**CROSS SECTION THRU ROADWAY - LOOKING NORTH**  
\* MEASURED NORMAL TO USH 18 SB POL



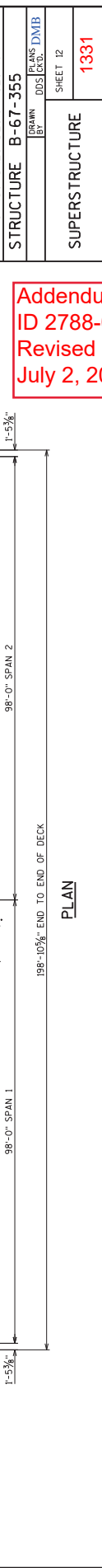
Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1331  
July 2, 2018

8

8

|                                                                                 |         |                   |     |          |
|---------------------------------------------------------------------------------|---------|-------------------|-----|----------|
| ADD                                                                             | 6/25/18 | ELECTRICAL DETAIL | MJK | BY       |
| NO.                                                                             | DATE    | REVISION          |     |          |
| STATE OF WISCONSIN<br>DEPARTMENT OF TRANSPORTATION<br>STRUCTURES DESIGN SECTION |         |                   |     |          |
| STRUCTURE <b>B-67-355</b>                                                       |         |                   |     |          |
| DESIGNED BY                                                                     |         | CHECKED BY        |     | DATE     |
| S509, S510                                                                      |         | S509, S510        |     | 6/25/18  |
| SUPERSTRUCTURE                                                                  |         |                   |     |          |
|                                                                                 |         |                   |     | SHEET 12 |
|                                                                                 |         |                   |     | 1331     |

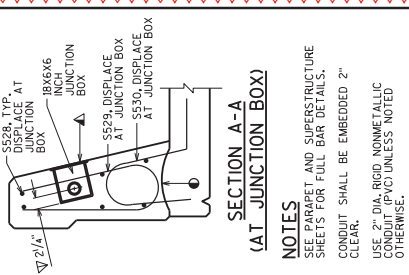
**PLAN**



1-2" DIA. CONDUIT  
IN WEST PARAPET  
DETAILS 5'-SHEET FOR  
DETAILS

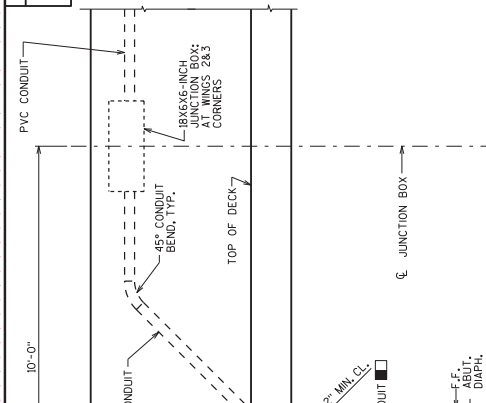
|   |      |        |
|---|------|--------|
| ★ | S519 | WING 1 |
| ★ | S520 | WING 2 |
| ★ | S532 | WING 2 |
| ★ | S533 | WING 3 |
| ★ | S519 | WING 3 |
| ★ | S520 | WING 3 |
| ★ | S532 | WING 4 |
| ★ | S533 | WING 4 |

STATE PROJECT NUMBER  
**2788-00-71**

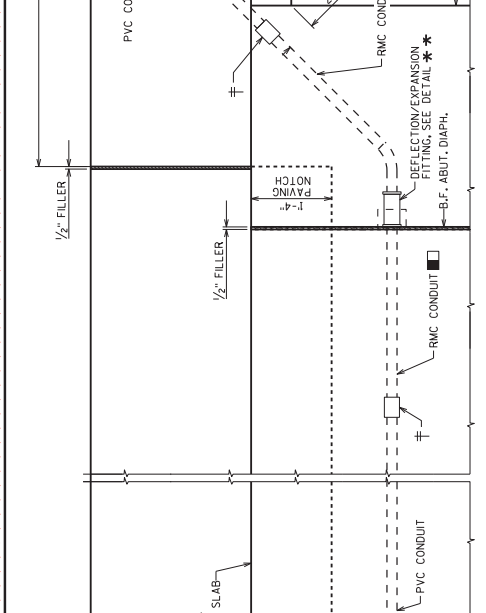


**NOTES**  
SEE PARAPET AND SUPERSTRUCTURE SHEETS FOR FULL BAR DETAILS.  
CONDUIT SHALL BE EMBEDDED 2" CLEAR.  
USE 2" DIA. RIGID NONMETALLIC UNLESS NOTED OTHERWISE.  
CONDUIT FITTINGS, CONDUIT BENDS, AND ADAPTER FITTINGS INCIDENTAL TO CONDUIT WORK.  
CONDUIT BENDS SHALL CONFORM TO THE NATIONAL ELECTRIC CODE.  
PROVIDE JUNCTION BOXES FROM THE APPROVED PRODUCTS LIST.

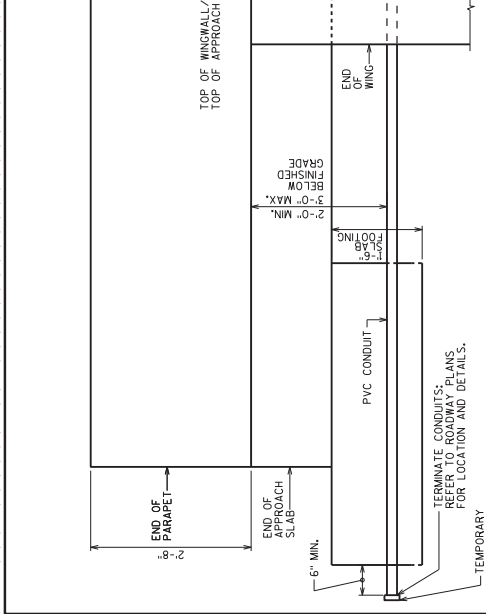
**LEGEND**  
 ○ CONSTRUCTION JOINT, STRIKE OFF AS SHOWN.  
 ▲ CUT OUT ± 1" OF GASKET AT BOTTOM OF JUNCTION BOX COVER TO ALLOW FOR DRAINAGE.  
 ▽ LOCATION OF CONDUIT IS MEASURED FROM OUTSIDE EDGE OF JUNCTION BOX.  
 † NONMETALLIC CONDUIT TO METALLIC CONDUIT ADAPTER FITTING UL OR SHALL BE USED FOR ELECTRICAL USE SHALL BE USED.  
 PVC = POLYVINYL CHLORIDE RIGID NONMETALLIC CONDUIT  
 RMC = RIGID METALLIC CONDUIT  
 ■ USE 2" DIA. RIGID METALLIC (RMC) CONDUIT AT FITTINGS. PROVIDE RMC FOR 3'-0" MIN. ON EACH SIDE OF JOINT OPENINGS UNLESS NOTED OTHERWISE.  
 ◀ SPONGE RUBBER WRAP TO BE AASHTO M 288.1 TYPE 1. PROVIDE MINIMUM THICKNESS OF 1/4" FOR THE ENTIRE LENGTH OF THE FITTING OR AS SHOWN. SPONGE WRAP SHALL BE 1/2" MIN. ON EACH SIDE OF JOINT OPENINGS UNLESS NOTED OTHERWISE.



**DEFLECTION/EXPANSION FITTING REQUIREMENTS:**  
UP TO 3° CONDUIT CONTRACTION OR EXPANSION AND UP TO 30 DEGREES OF EXPANSION SHALL BE ALLOWED IN ANY DIRECTION WITH BONDING JUMPER



**OUTSIDE ELEVATION VIEW OF WINGWALL, STRUCTURAL APPROACH SLAB & PARAPET SHOWING CONDUITS**  
(WING 3 SHOWN, WING 2 SIMILAR)



**PLAN VIEW OF WINGWALL, STRUCTURAL APPROACH SLAB & PARAPET SHOWING CONDUITS**  
(WING 3 SHOWN, WING 2 SIMILAR)



**DEFLECTION/EXPANSION FITTING DETAIL**  
BOND JUMPER NOT SHOWN FOR CLARITY

|                                                                                 |                       |     |          |
|---------------------------------------------------------------------------------|-----------------------|-----|----------|
| 6/14/18                                                                         | ADD ELECTRICAL DETAIL | MLK | BY       |
| NOI                                                                             | REVISION              |     |          |
| STATE OF WISCONSIN<br>DEPARTMENT OF TRANSPORTATION<br>STRUCTURES DESIGN SECTION |                       |     |          |
| STRUCTURE B-67-355                                                              |                       |     |          |
| BRN                                                                             | MLK                   | CLD | DMB      |
| CONDUIT DETAILS                                                                 |                       |     | SHEET 19 |
|                                                                                 |                       |     | 1337A    |

Addendum No. 01  
ID 2788-00-71  
Added Sheet 1337A  
July 2, 2018

*William C. Decker*  
6/25/18

**DESIGN DATA**  
 LIVE LOAD:  
 DESIGN LOADING: HL-93  
 INVENTORY RATING FACTOR: RF=1.11  
 OPERATING RATING FACTOR: RF=L44  
 WISCONSIN STANDARD PERMIT VEHICLE (MS-SPV): 250 (NIPS)  
 STRUCTURE IS DESIGNED FOR A FUTURE WEARING SURFACE OF 20 POUNDS PER SQUARE FOOT.  
**MATERIALS PROPERTIES:**  
 CONCRETE MASONRY: SUPERSTRUCTURE— $f_c=4,000$  P.S.I. ALL OTHER— $f_c = 3,500$  P.S.I.  
 BAR STEEL REINFORCEMENT, GRADE 60  $f_y = 60,000$  P.S.I.  
 36W\* PRESTRESSED GIRDERS, CONCRETE MASONRY  $f_c = 8,000$  P.S.I.  
 STRANDS—.06" DIA. WITH ULTIMATE TENSILE STRENGTH OF 270,000 P.S.I.

**FOUNDATION DATA**  
 ABUTMENTS TO BE SUPPORTED ON HP 12-INCH X 53 LB STEEL PILING DRIVEN TO A REQUIRED DRIVING RESISTANCE OF 220 TONS\*\* PER PILE AS DETERMINED BY THE MODIFIED GATES DYNAMIC FORMULA. ESTIMATED 35'-0" LONG PILES AT THE SOUTH ABUTMENT WITH PILE POINTS REQUIRED. ESTIMATED 40'-0" LONG PILES AT THE NORTH ABUTMENT WITH PILE POINTS REQUIRED.  
 PIER TO BE SUPPORTED ON 4" DIA. X 0.5-INCH THICK CAST-IN-PLACE CONCRETE PILING DRIVEN TO A REQUIRED DRIVING RESISTANCE OF 240 TONS\*\* PER PILE AS DETERMINED BY THE MODIFIED GATES DYNAMIC FORMULA. ESTIMATED 45'-0" LONG PILES AT THE PIER WITH PILE POINTS REQUIRED.  
 \*\* THE FACTORED AXIAL RESISTANCE OF PILES IN COMPRESSION USED FOR DESIGN OF THE UNDERMINED RESISTANCE SHALL BE DETERMINED BY THE GATEWAY FACTOR OF 0.5 USING MODIFIED GATES TO DETERMINE DOWN PILE CAPACITY.

**TRAFFIC VOLUME**  
 U.S. 18  
 A.A.D.T.=17,300 (2037)  
 R.D.S.=50 MP.A.H.

**CURVE DATA**  
 R = U.S. 18  
 P.L. = 125+62.72  
 $\Delta = 27^{\circ}48'-49"$  RT  
 $D = 60'$   
 $L = 189.77$   
 $R = 5,000$   
 $S.E. = 2.0\%$   
 $P.T. = 135+00.04$

**LIST OF DRAWINGS**  
 1. GENERAL PLAN  
 2. GROSS SECTION & QUANTITIES  
 3. SUBSURFACE EXPLORATION  
 4. SOUTH ABUTMENT  
 5. SOUTH ABUTMENT DETAILS  
 6. NORTH ABUTMENT  
 7. NORTH ABUTMENT DETAILS  
 8. PIER  
 9. 36W\* PRESTRESSED GIRDER DETAILS 1  
 10. 36W\* PRESTRESSED GIRDER DETAILS 2  
 11. STEEL DIAPHRAGM  
 12. SUPERSTRUCTURE CROSS SECTIONS  
 13. PIER DIAPHRAGM  
 14. BECK LAYOUT  
 15. SUPERSTRUCTURE PLAN  
 16. DECK LAYOUT  
 17. STRUCTURAL APPROACH SLAB & BAR DETAILS  
 18. STRUCTURAL APPROACH SLAB & BAR DETAILS  
 19. SINGLE SLOPE PARAPET 3255 (WEST)  
 20. MODIFIED SINGLE SLOPE PARAPET 3255 (EAST)  
 21. SUPERSTRUCTURE BAR DETAILS  
 22. CONDUIT DETAILS

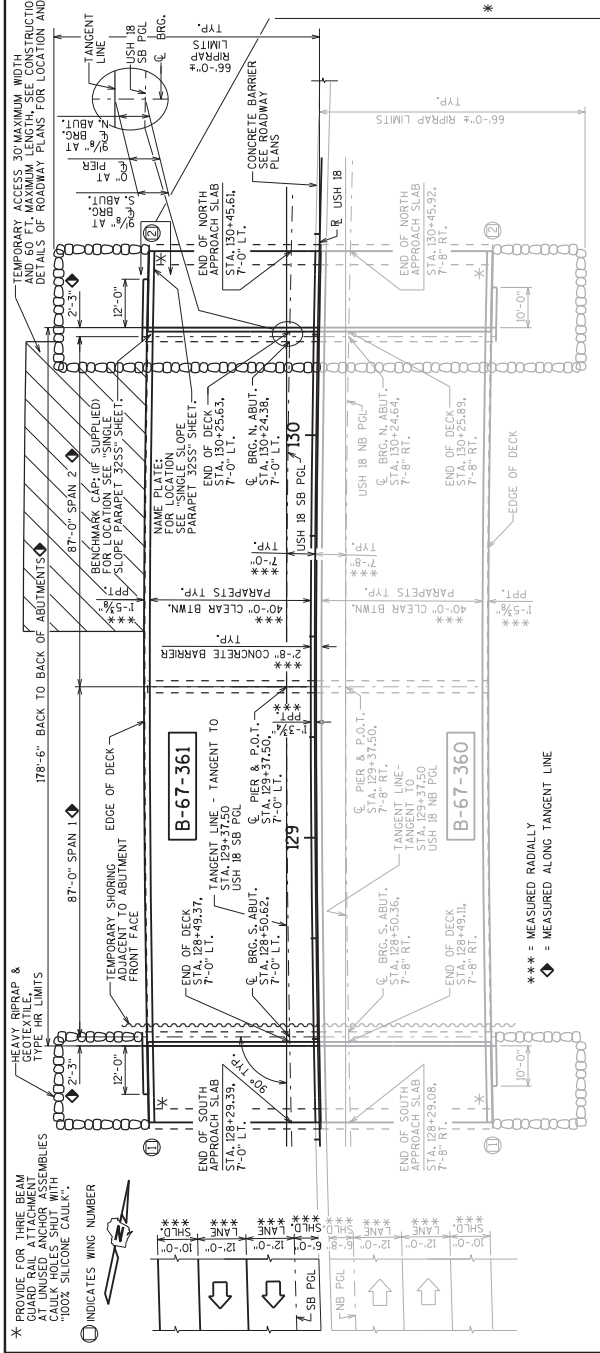
ACCEPTED: **WISDOT**  
 BUREAU OF STRUCTURES  
 11/11/17  
 CHIEF STRUCTURES DESIGN ENGINEER

|                        |          |    |
|------------------------|----------|----|
| ADD. ELECTRICAL DETAIL | MJK      | BY |
| NO. DATE               | REVISION |    |
| 6/19/18                |          |    |

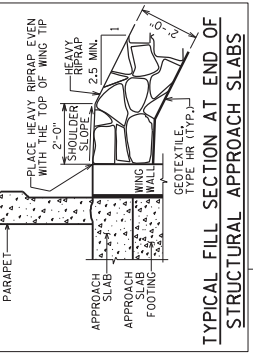
STRUCTURE DESIGN CONTACT:  
 WALT COUPAR (608) 266-5083  
 AARON BOK (608) 261-0261

Plans Prepared by **WISDOT**  
**BUREAU OF STRUCTURES**  
*William C. Decker*  
 6/25/18  
 CHIEF STRUCTURES DESIGN ENGINEER

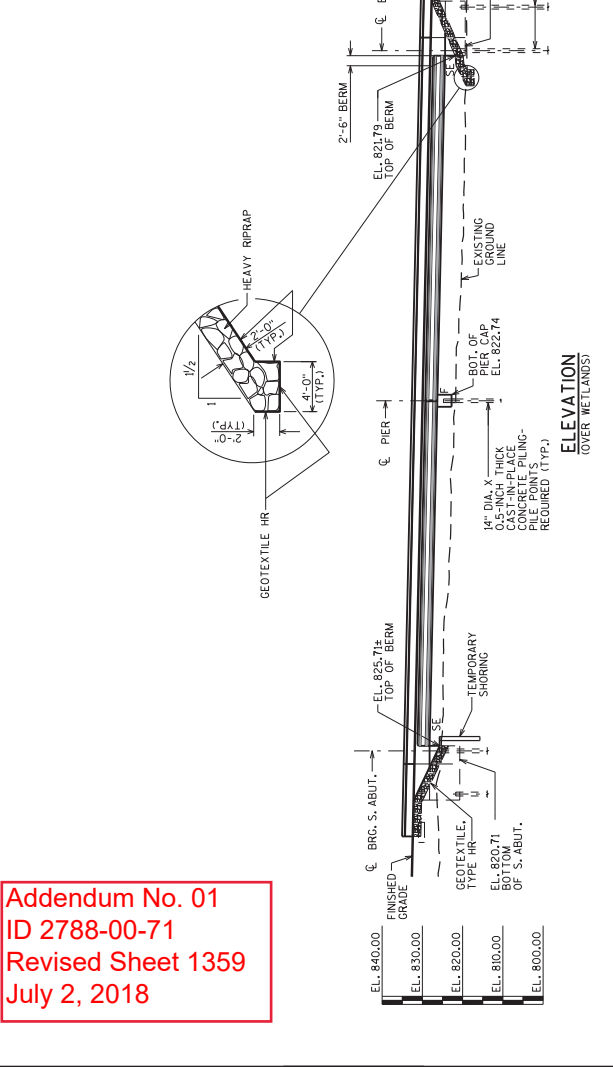
STRUCTURE **B-67-361**  
 U.S. 18 SOUTHBOUND LANES OVER WETLANDS  
 COUNTY: WALKESHA TOWN: WALKESHA  
 DESIGN SPEC. NO. 360  
 WISCONSIN STANDARD PERMIT VEHICLE (MS-SPV): 250 (NIPS)  
 DESIGNED: MSC/EXR DRAWN BY: JPH/EXR  
 SHEET 1 OF 22  
**GENERAL PLAN**  
**1359**



**PLAN**  
 2 SPAN - 36W\* PRESTRESSED GIRDER  
 \*\*\* = MEASURED RADIALLY  
 ◆ = MEASURED ALONG TANGENT LINE

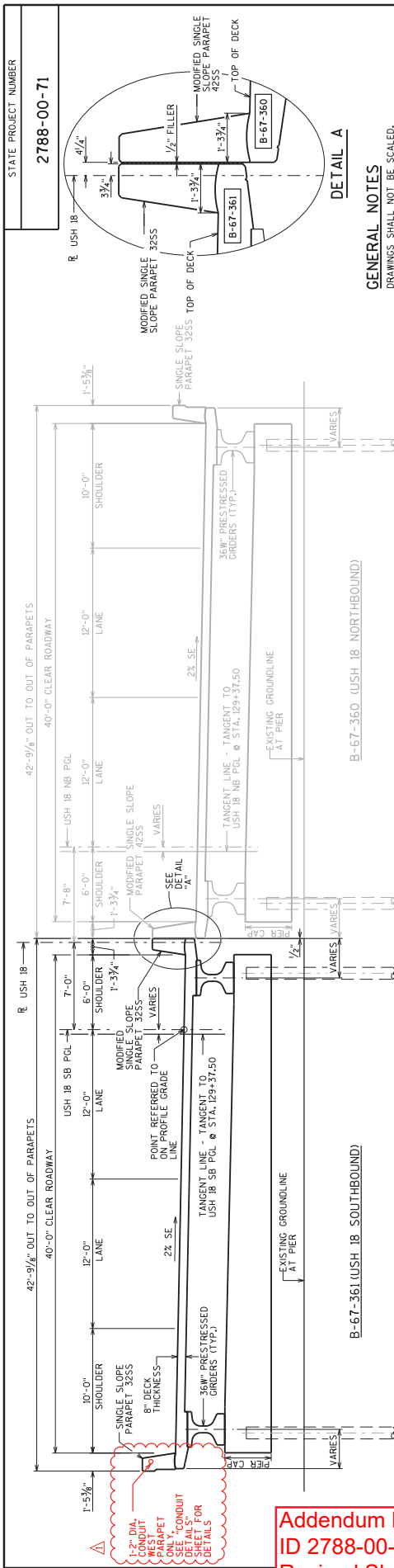


**TYPICAL FILL SECTION AT END OF STRUCTURAL APPROACH SLABS**  
 1. GENERAL PLAN  
 2. GROSS SECTION & QUANTITIES  
 3. SUBSURFACE EXPLORATION  
 4. SOUTH ABUTMENT  
 5. SOUTH ABUTMENT DETAILS  
 6. NORTH ABUTMENT  
 7. NORTH ABUTMENT DETAILS  
 8. PIER  
 9. 36W\* PRESTRESSED GIRDER DETAILS 1  
 10. 36W\* PRESTRESSED GIRDER DETAILS 2  
 11. STEEL DIAPHRAGM  
 12. SUPERSTRUCTURE CROSS SECTIONS  
 13. PIER DIAPHRAGM  
 14. BECK LAYOUT  
 15. SUPERSTRUCTURE PLAN  
 16. DECK LAYOUT  
 17. STRUCTURAL APPROACH SLAB & BAR DETAILS  
 18. STRUCTURAL APPROACH SLAB & BAR DETAILS  
 19. SINGLE SLOPE PARAPET 3255 (WEST)  
 20. MODIFIED SINGLE SLOPE PARAPET 3255 (EAST)  
 21. SUPERSTRUCTURE BAR DETAILS  
 22. CONDUIT DETAILS



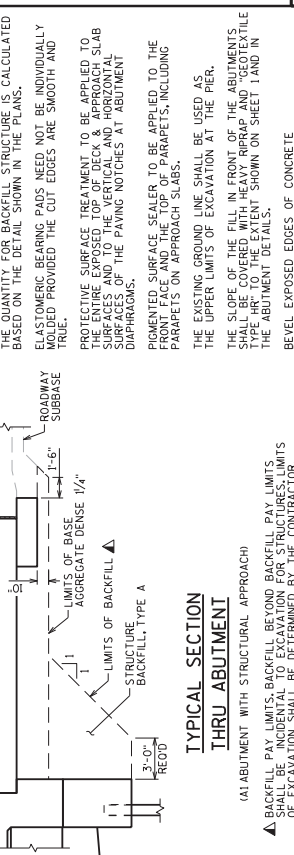
**ELEVATION**  
 (OVER WETLANDS)

Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1359  
 July 2, 2018

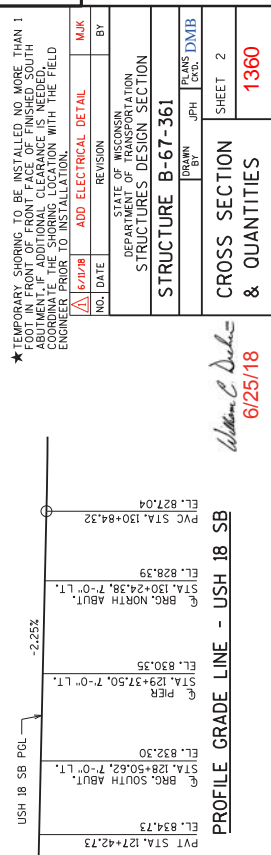


**CROSS SECTION THRU ROADWAY LOOKING NORTH (UPSTATION)**  
 ALL HORIZONTAL DIMENSIONS ARE MEASURED RADIIALLY - NORMAL TO THE CURVED "R" U.S. 18"

**GENERAL NOTES**  
 DRAWINGS SHALL NOT BE SCALED.  
 BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2" CLEAR UNLESS OTHERWISE SHOWN OR NOTED.  
 THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE.  
 AT THE BACK FACE OF ABUTMENT ALL VOLUME WHICH CANNOT BE OCCUPIED BEFORE ABUTMENT CONSTRUCTION AND IS NOT OCCUPIED BY THE NEW STRUCTURE SHALL BE BACKFILLED WITH STRUCTURE BACKFILL. ALSO AS DETAILED ON THE STRUCTURAL APPROACH SLAB SHEETS.  
 THE QUANTITY FOR BACKFILL STRUCTURE IS CALCULATED BASED ON THE DETAIL SHOWN IN THE PLANS.  
 ELASTOMERIC BEARING PADS NEED NOT BE INDIVIDUALLY MOLDED PROVIDED THE CUT EDGES ARE SMOOTH AND TRUE.  
 PROTECTIVE SURFACE TREATMENT TO BE APPLIED TO THE ENTIRE EXPOSED TOP OF DECK & APPROACH SLAB SURFACES AND TO THE VERTICAL AND HORIZONTAL DIAPHRAGMS. THE FINING NOTICES AT ABUTMENT RICHENED SURFACE SEALERS TO BE APPLIED TO THE FRONT FACE AND THE TOP OF PARAPETS, INCLUDING PARAPETS ON APPROACH SLABS.  
 THE EXISTING GROUND LINE SHALL BE USED AS THE UPPER LIMITS OF EXCAVATION AT THE PIER.  
 THE SLOPE OF THE FILL IN FRONT OF THE ABUTMENTS SHALL BE COVERED WITH HEAVY RIPRAP AND "GEO-TEXTILE" THE ABUTMENT DETAILS.  
 BEVEL EXPOSED EDGES OF CONCRETE UNLESS OTHERWISE NOTED.  
 THE HAUNCH CONCRETE QUANTITY IS BASED ON THE AVERAGE HAUNCH SHOWN ON THE 'ORDER DETAILS 2' SHEET.  
 \*TEMPORARY SHORING TO BE INSTALLED NO MORE THAN 1' FROM THE FRONT FACE OF THE CONCRETE ABUTMENT. THE SHORING LOCATION WITH THE FIELD ENGINEER PRIOR TO INSTALLATION.



**TYPICAL SECTION THRU ABUTMENT**  
 (AT ABUTMENT WITH STRUCTURAL APPROACH)  
 ▲ BACKFILL PAY LIMITS, BACKFILL BEYOND BACKFILL PAY LIMITS SHALL BE DETERMINED BY THE CONTRACTOR.



**CROSS SECTION & QUANTITIES**  
 SHEET 2  
**1360**

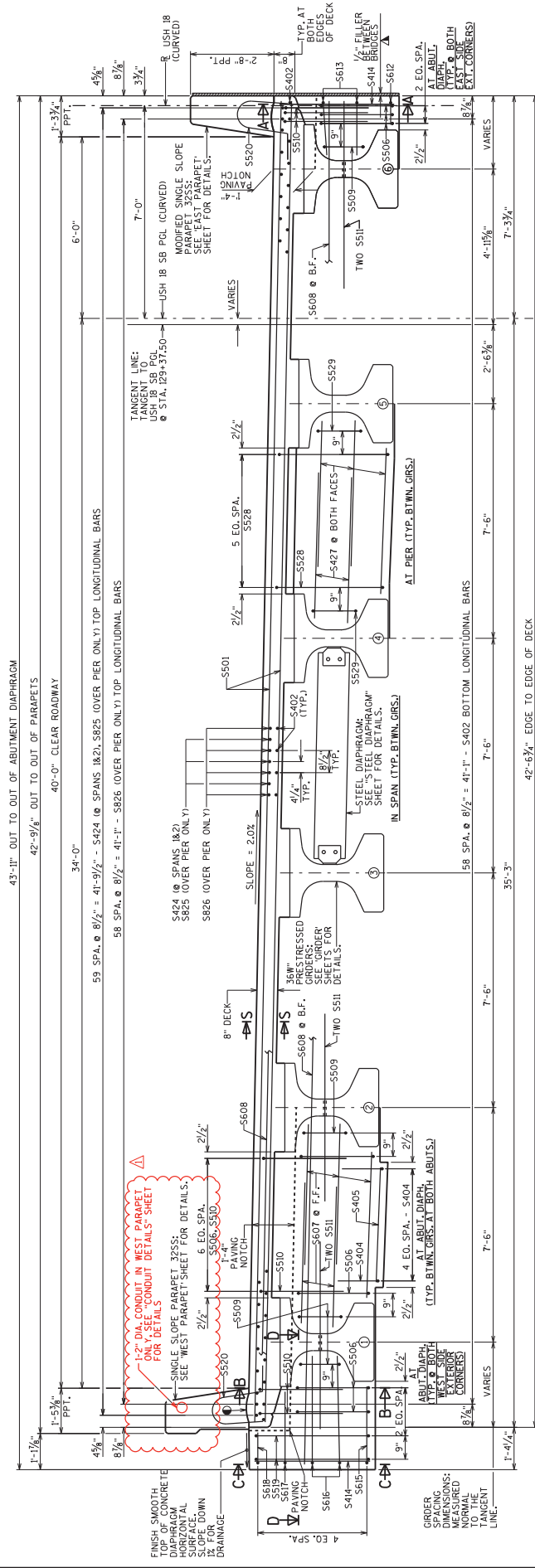
**TOTAL ESTIMATED QUANTITIES**

| BID ITEM NUMBER | BID ITEMS                                       | UNIT | SUPER. | SOUTH ABUT. | PIER  | NORTH ABUT. | APPROACH SLAB | TOTALS           |
|-----------------|-------------------------------------------------|------|--------|-------------|-------|-------------|---------------|------------------|
| 206.0000        | EXCAVATION FOR STRUCTURES BRIDGES B-67-361      | LS   |        |             |       |             |               | 1                |
| 210.0000        | BACKFILL STRUCTURE TYPE A                       | TON  |        | 190         |       | 110         |               | 300              |
| 305.0220        | BASE AGGREGATE DENSE 1 1/4-INCH                 | TON  |        | 134         |       | 134         |               | 268              |
| 501.0000.S      | ICE HOT WEATHER CONCRETING                      | LB   | 2,228  | 442         | 127   | 308         |               | 3,985            |
| 502.0100        | CONCRETE MASONRY BRIDGES                        | CY   | 297    | 59          | 17    | 41          |               | 532              |
| 502.3200        | PROTECTIVE SURFACE TREATMENT                    | SY   | 810    | 90          |       | 90          |               | 990              |
| 502.3210        | PIGMENTED SURFACE SEALER                        | SY   | 145    |             |       | 17          |               | 179              |
| 503.0137        | PRESTRESSED GRIDER TYPE 1, 36W-INCH             | LF   | 1,049  |             |       |             |               | 1,049            |
| 505.0400        | BAR STEEL REINFORCEMENT HS STRUCTURES           | LB   |        | 3,050       |       | 2,820       |               | 5,870            |
| 505.0600        | BAR STEEL REINFORCEMENT HS COATED STRUCTURES    | LB   | 62,875 | 965         | 5,460 | 810         |               | 89,850           |
| 505.0800.S      | BAR STEEL REINFORCEMENT HS STAINLESS STRUCTURES | LB   | 1,430  |             |       |             |               | 1,430            |
| 506.2605        | BEARING PADS ELASTOMERIC NON-LAMINATED          | EACH | 24     |             |       |             |               | 24               |
| 506.4000        | STEEL DIAPHRAGMS B-67-361                       | EACH | 20     |             |       |             |               | 20               |
| 511.2000        | TEMPORARY SHORING B-67-361                      | SF   |        | 125         |       |             |               | 125              |
| 516.0500        | RUBBERIZED MEMBRANE WATERPROOFING               | SY   |        | 11          |       | 11          |               | 22               |
| 550.0500        | PILE POINTS                                     | EACH |        | 8           | 13    | 8           |               | 29               |
| 550.1120        | PILE STEEL HP 12-INCH X 53 LB                   | LF   |        | 280         |       | 320         |               | 600              |
| 550.2448        | PILE STEEL HP 12-INCH X 53 LB                   | LF   |        | 280         |       | 320         |               | 600              |
| 606.0300        | RRAPRAP HEAVY                                   | CY   |        | 45          | 585   | 90          |               | 685              |
| 612.0406        | PIPE UNDERDRAIN WRAPPED 6-INCH                  | LF   |        | 73          |       | 73          |               | 146              |
| 614.0150        | ANCHOR ASSEMBLIES FOR STEEL PLATE BEAM GUARD    | EACH | 2      |             |       |             |               | 2                |
| 646.0120        | GEOTEKSTILE TYPE-A-R                            | SF   |        | 70          |       | 180         |               | 250              |
| 652.0125        | CONDUIT RIGID METALLIC 2-INCH                   | LF   |        | 10          |       | 10          |               | 20               |
| 652.0225        | CONDUIT RIGID NONMETALLIC SCHEDULE 40 2-INCH    | LF   |        | 180         |       | 23          |               | 203              |
| 653.0220        | JUNCTION BOXES 18X18-INCH                       | EACH | 2      |             |       |             |               | 2                |
|                 | NON-BID ITEMS                                   |      |        |             |       |             |               |                  |
|                 | FILLER                                          | SIZE |        |             |       |             |               | 1/2" - 3/4" 1/2" |

**Addendum No. 01**  
**ID 2788-00-71**  
**Revised Sheet 1360**  
**July 2, 2018**

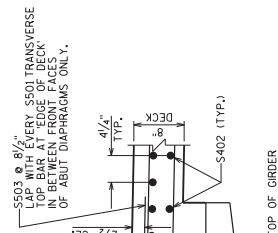
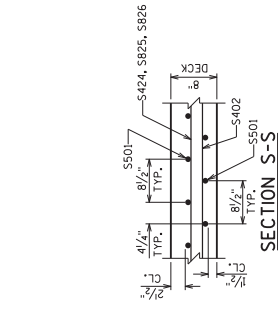


STATE PROJECT NUMBER  
**2788-00-71**



**CROSS SECTION THRU BRIDGE LOOKING NORTH**  
 (ALL HORIZONTAL DIMENSIONS ARE MEASURED RADIIALLY - NORMAL TO THE CURVED USH 18 SB PCL - EXCEPT FOR THE ORDER SPACING DIMENSIONS.)  
 (NORTH ABUTMENT SHOWN, SOUTH ABUTMENT SIMILAR)

**Addendum No. 01**  
**ID 2788-00-71**  
**Revised Sheet 1370**  
**July 2, 2018**



NOTES:  
 FOR THE SECTIONS THRU THE ABUTMENTS SEE THE "ABUTMENT DIAPHRAGM" SHEET.  
 SEE "ABUTMENT DIAPHRAGM" SHEET FOR ABUTMENT DIAPHRAGM CORNER DETAILS.  
 FOR THE SECTION THRU THE PIER DIAPHRAGM SEE "PIER DIAPHRAGM" SHEET.  
 ● HORIZ. CONST. JOINT: STRIKE OFF AND LEAVE ROUGH AS SHOWN  
 ▲ SEAL ALL EXPOSED HORIZONTAL AND VERTICAL SURFACES OF 1/2" FILLER WITH 3" WIDE CONCRETE JOINT SEALER. IT DEEP AND HOLD 1/4" BELOW SURFACE OF CONCRETE. EXTEND SEALER 3" BELOW GUTTER LINE AT INSIDE FACE.

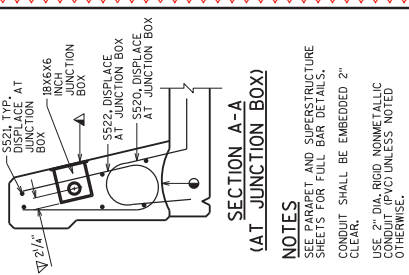
|         |      |                       |    |
|---------|------|-----------------------|----|
| NO.     | DATE | ADD ELECTRICAL DETAIL | BY |
| 6/28/11 |      |                       |    |

|                           |         |                              |      |
|---------------------------|---------|------------------------------|------|
| STATE OF WISCONSIN        |         | DEPARTMENT OF TRANSPORTATION |      |
| STRUCTURES DESIGN SECTION |         | STRUCTURE B-67-361           |      |
| DESIGNED BY               | DRWN BY | CHECKED BY                   | DATE |
|                           |         |                              |      |

|                |  |          |  |
|----------------|--|----------|--|
| SUPERSTRUCTURE |  | SHEET 12 |  |
| CROSS SECTIONS |  | 1370     |  |

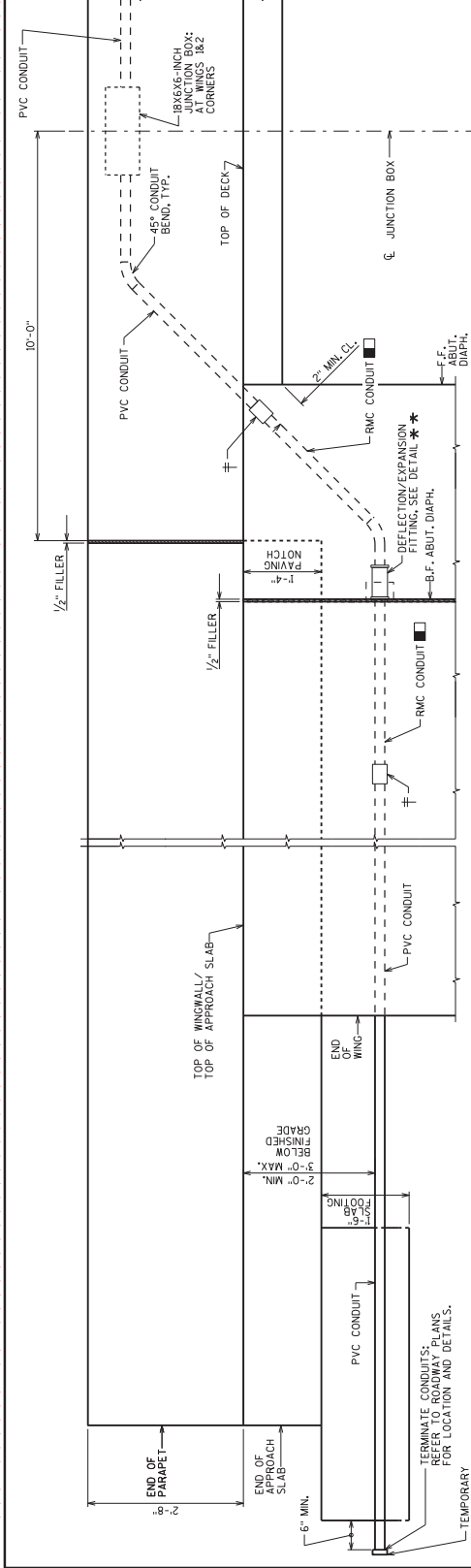
*William C. Decker*  
 6/25/18

STATE PROJECT NUMBER  
**2788-00-71**

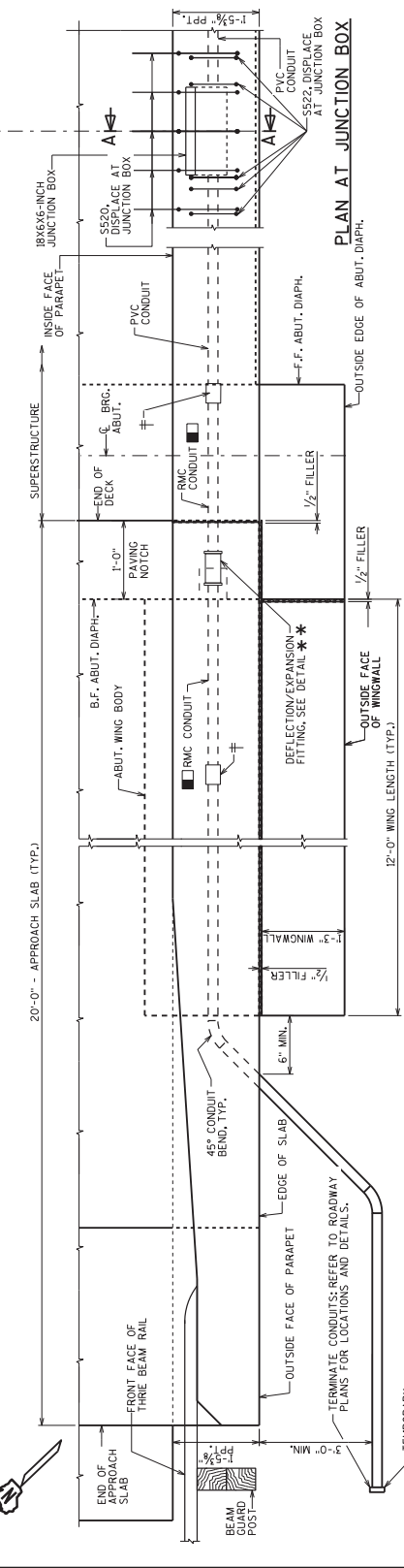


**NOTES**  
SEE PARAPET AND SUPERSTRUCTURE SHEETS FOR FULL BAR DETAILS.  
CONDUIT SHALL BE EMBEDDED 2" CLEAR.  
USE 2" DIA. RIGID NONMETALLIC UNLESS NOTED OTHERWISE.  
CONDUIT FITTINGS, CONDUIT BENDS, AND ADAPTER FITTINGS INCIDENTAL TO CONDUIT WORK.  
CONDUIT BENDS SHALL CONFORM TO THE NATIONAL ELECTRIC CODE.  
PROVIDE JUNCTION BOXES FROM THE APPROVED PRODUCTS LIST.

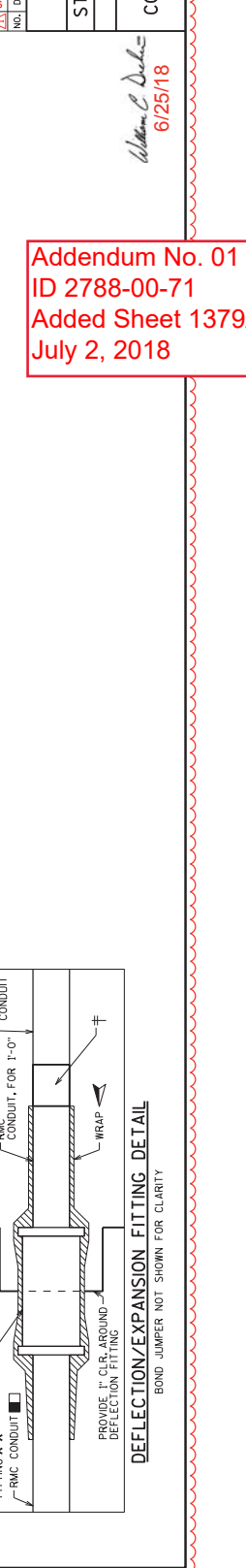
**LEGEND**  
 ○ CONSTRUCTION JOINT, STRIKE OFF AS SHOWN.  
 ▲ CUT OUT ± 1" OF GASKET AT BOTTOM OF JUNCTION BOX COVER TO ALLOW FOR DRAINAGE.  
 ▽ LOCATION OF CONDUIT IS MEASURED FROM OUTSIDE EDGE OF JUNCTION BOX.  
 † NONMETALLIC CONDUIT TO METALLIC CONDUIT ADAPTER FITTING UL OR SHALL BE USED FOR ELECTRICAL USE SHALL BE USED.  
 PVC = POLYVINYL CHLORIDE RIGID NONMETALLIC CONDUIT  
 RMC = RIGID METALLIC CONDUIT  
 ■ USE 2" DIA. RIGID METALLIC (RMC) CONDUIT AT FITTINGS. PROVIDE RMC FOR 3'-0" MIN. ON EACH SIDE OF JOINT OPENINGS UNLESS NOTED OTHERWISE.  
 ← SPONGE RUBBER WRAP TO BE AASHTO M 288 TYPE 1. PROVIDE MINIMUM THICKNESS OF 1/4" FOR THE ENTIRE LENGTH OF THE FITTING OR AS SHOWN. SPONGE WRAP SHALL BE METALLIC 2-INCH.



**OUTSIDE ELEVATION VIEW OF WINGWALL, STRUCTURAL APPROACH SLAB & PARAPET SHOWING CONDUITS**  
(WING 2 SHOWN, WING 1 SIMILAR)



**PLAN VIEW OF WINGWALL, STRUCTURAL APPROACH SLAB & PARAPET SHOWING CONDUITS**  
(WING 2 SHOWN, WING 1 SIMILAR)



**DEFLECTION/EXPANSION FITTING REQUIREMENTS:**  
UP TO 3/4" CONDUIT CONTRACTION OR EXPANSION AND UP TO 30 DEGREES OF ANGLE IN ANY DIRECTION WITH BONDING JUMBER

**Addendum No. 01**  
**ID 2788-00-71**  
**Added Sheet 1379A**  
**July 2, 2018**

*William C. Decker*  
**6/25/18**

|                                                                                 |                       |     |              |
|---------------------------------------------------------------------------------|-----------------------|-----|--------------|
| 6/14/18                                                                         | ADD ELECTRICAL DETAIL | MLK | BY           |
| NOI DATE                                                                        | REVISION              |     |              |
| STATE OF WISCONSIN<br>DEPARTMENT OF TRANSPORTATION<br>STRUCTURES DESIGN SECTION |                       |     |              |
| <b>STRUCTURE B-67-361</b>                                                       |                       |     |              |
| BRN                                                                             | MLK                   | CRD | DMB          |
| CONDUIT DETAILS                                                                 |                       |     | SHEET 22     |
|                                                                                 |                       |     | <b>1379A</b> |

**GENERAL NOTES**

DRAWINGS SHALL NOT BE SCALED.  
THE CONTRACTOR SHALL PROVIDE COMPLETE DESIGN, PLANS, DETAILS, SPECIFICATIONS AND SHOP DRAWINGS FOR THE DIVISIONS. THE RETAINING WALL MANUFACTURER SHALL PROVIDE TECHNICAL ASSISTANCE TO THE CONTRACTOR DURING CONSTRUCTION. THE ITEMS "WALL MODULAR BLOCK MECHANICALLY STABILIZED EARTH R-67-145" AND "WALL MODULAR BLOCK GRAVITY R-67-145". PLANS, ELEVATIONS AND DETAILS GIVEN ON THESE DRAWINGS ARE INTENDED TO INDICATE WALL LOCATIONS, LENGTHS, HEIGHTS, AND DETAILS COMMON TO THE WALL SYSTEM SELECTED. THE CONTRACTOR SHALL VERIFY THAT THE WALL SYSTEM, INCLUDING THE REQUIRED ALIGNMENTS AND DETAILS, THE RETAINING WALL IS TO BE DESIGNED USING THE ELEVATIONS GIVEN ON THIS SHEET AND ON THE "WALL DETAILS AND ELEVATIONS" SHEET. DESIGN FOR RETAINING WALL TO PROVIDE FOR FINISHED GRADE SLOPE. DESIGN RETAINING WALL FOR A LIVE LOAD SURCHARGE OF 100 PSF. ALL WALL STATIONING AND OFFSETS ARE GIVEN AT THE FRONT FACE OF WALL R-67-145.  
THE CONTRACTOR SHALL REMOVE 12 INCHES OF MATERIAL BELOW THE BASE BLOCK AND REPLACE WITH GRANULAR BACKFILL IN THE AREAS THAT TOPSOIL, SILT AND CLAY ARE ENCOUNTERED, AS RECOMMENDED IN THE GEOTECHNICAL REPORT.  
THE MAXIMUM VALUE OF THE INTERNAL ANGLE OF FRICTION OF THE WALL BACKFILL MATERIAL IN THE REINFORCED ZONE SHALL BE ASSUMED TO BE 30° WITHOUT CERTIFIED TEST VALUES.

**ALLOWABLE WALL SYSTEMS**

WALL MODULAR BLOCK GRAVITY  
WALL MODULAR BLOCK MECHANICALLY STABILIZED EARTH

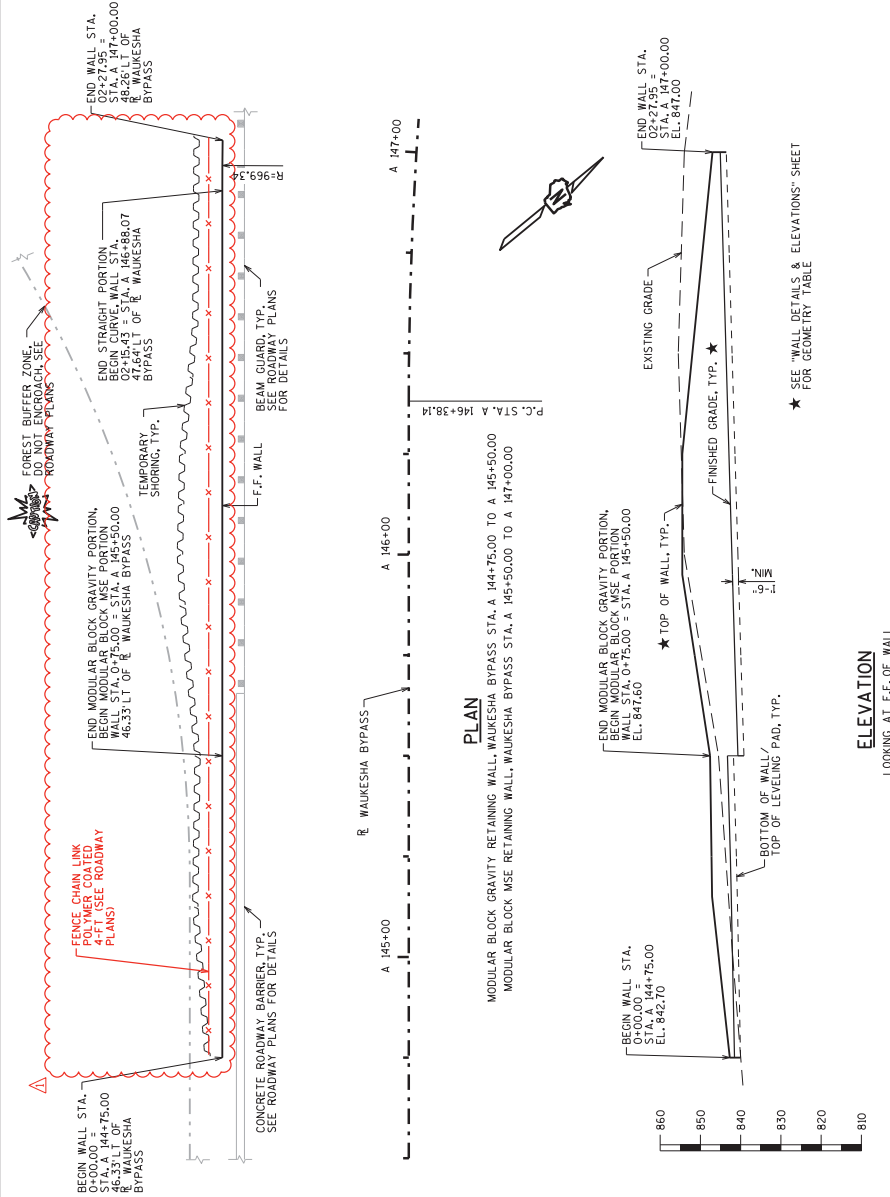
**CURVE 4 DATA**

WAIKESHA BYPASS  
P.I. = A 149+23.38  
Δ = 84°32'26"  
T = 285.24  
L = 555.72  
R = 1000.00  
S.C. = 4.02  
P.T. = A 151+35.86

**LIST OF DRAWINGS**

1. GENERAL PLAN  
2. WALL DETAILS & ELEVATIONS  
3. SUBSURFACE EXPLORATION

STRUCTURE DESIGN CONTACTS:  
MAXWELL KULLICK (608) 261-6108  
AARON BONK (608) 261-0261



Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1406  
July 2, 2018

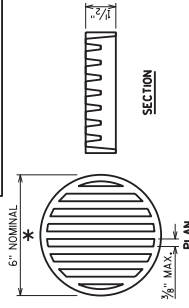
**TOTAL ESTIMATED QUANTITIES**

| BID ITEM NUMBER | BID ITEMS                                                 | UNIT | TOTALS |
|-----------------|-----------------------------------------------------------|------|--------|
| 511200          | TEMPORARY SHORING R-67-145                                | SF   | 2,080  |
| 612.0406        | PIPE UNDERDRAIN WRAPPED 6-INCH                            | LF   | 250    |
| SPV.0165.03     | WALL MODULAR BLOCK MECHANICALLY STABILIZED EARTH R-67-145 | SF   | 1,544  |
| SPV.0165.04     | WALL MODULAR BLOCK GRAVITY R-67-145                       | SF   | 392    |

*William C. Decker*  
6/25/18

6/25/18 ADD FENCING MJK  
NO. DATE REVISION BY  
**BUREAU OF STRUCTURES**  
ACCEPTED *William C. Decker* 6/23/17 DATE  
STRUCTURE R-67-145  
COUNTY WAIKESHA TOWN WAIKESHA  
DESIGN SPEC. WASHINGTON BRIDGE DESIGN SPECIFICATIONS  
DESIGNED BY WAIKESHA COUNTY MJK (C.D.)  
CHECKED BY MJK (C.D.)  
GENERAL PLAN  
SHEET 1 OF 3  
**1406**

STATE PROJECT NUMBER  
**2788-00-71**



**RODENT SHIELD DETAIL**

\* DIMENSIONS ARE APPROXIMATE; THE GRATE IS SIZED TO FIT INTO A PIPE COUPLING. ORIENT SO SLOTS ARE VERTICAL.  
THE RODENT SHIELD, PIPE COUPLING AND SCREENS SHALL BE CONSIDERED INCIDENTAL TO THE BID ITEM "PIPE UNDERDRAIN WRAPPED 6-INCH".  
THE RODENT SHIELD SHALL BE A PVC GRATE SIMILAR TO THIS DETAIL. THE GRATE IS COMMERCIALY AVAILABLE AS A FLOOR STRAINER. A PIPE COUPLING IS REQUIRED FOR THE ATTACHMENT OF THIS SHIELD TO THE UNDERDRAIN. THE PIPE COUPLING SHALL BE FASTENED TO THE PIPE COUPLING WITH TWO OR MORE NO. 10 X 1-INCH STAINLESS STEEL SHEET METAL SCREWS.

(A15) PIPE UNDERDRAIN WRAPPED 6-INCH; SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. RODENT SHIELD REQUIRED.

(A16) PROVIDE SLEEVE FOR FENCE POST, FOOTING IF REQUIRED. PROVIDE SLEEVES INCIDENTAL TO WALL ITEMS.

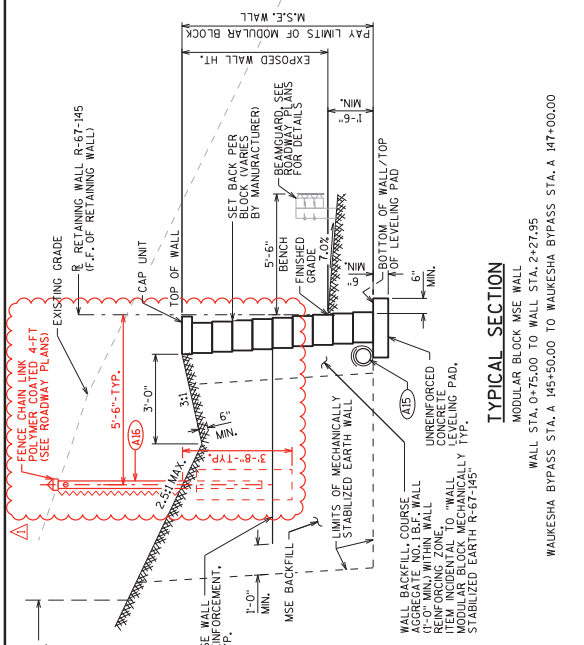
**Addendum No. 01**  
**ID 2788-00-71**  
**Revised Sheet 1407**  
**July 2, 2018**

|     |         |     |       |          |     |
|-----|---------|-----|-------|----------|-----|
| NO. | DATE    | ADD | FENCE | REVISION | BY  |
| 1   | 6/29/18 |     |       |          | MJK |

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION  
**STRUCTURES DESIGN SECTION**  
**STRUCTURE R-67-145**

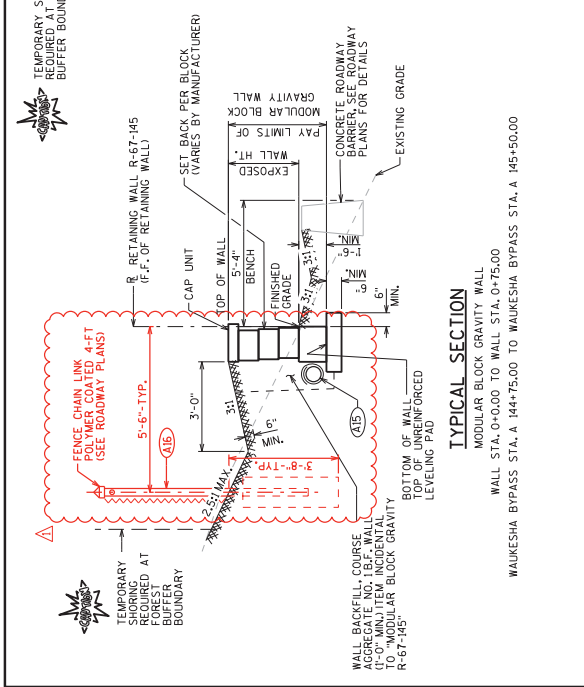
WALL DETAILS & ELEVATIONS  
SHEET 2  
**1407**

*William C. Decker*  
**6/25/18**



**TYPICAL SECTION**

MODULAR BLOCK MSE WALL  
WALL STA. 0+75.00 TO WALL STA. 2+27.95  
WAUKESHA BYPASS STA. A 145+50.00 TO WAUKESHA BYPASS STA. A 147+00.00



**TYPICAL SECTION**

MODULAR BLOCK GRAVITY WALL  
WALL STA. 0+0.00 TO WALL STA. 0+75.00  
WAUKESHA BYPASS STA. A 144+75.00 TO WAUKESHA BYPASS STA. A 145+50.00

**WALL EXTERNAL & OVERALL STABILITY EVALUATION**

| PARAMETER                                   | VALUES                                  |
|---------------------------------------------|-----------------------------------------|
| WALL HEIGHT (FEET) <sup>1</sup>             | 9.1                                     |
| EXPOSED WALL HEIGHT (FEET)                  | 7.6                                     |
| MINIMUM LENGTH OF REINFORCEMENT (FEET)      | 6.0                                     |
| BORING LOCATION USED                        | BOR - 1 BOR - 2                         |
| APPROXIMATE R                               | WAUKESHA BYPASS STATION A145+25 A146+95 |
| CAPACITY TO DEMAND RATIO (CDR) <sup>2</sup> | L2 L2                                   |
| SLIDING (CDR > 1.0)                         | L3 L3                                   |
| ECCENTRICITY (CDR > 1.0)                    | NA <sup>3</sup> L1                      |
| BEARING RESISTANCE (CDR > 1.0)              | L9 L3                                   |
| FACTORED BEARING RESISTANCE (PSF)           | 5,500 5,500                             |

NOTES:  
1. THE WALL HEIGHT INCLUDES EMBEDMENT OF 1.5 FEET.  
2. CDR REQUIREMENTS AND LOAD AND RESISTANCE FACTORS ARE AS APPLICABLE TO THIS PROJECT.  
3. NA NOT APPLICABLE. THE GLOBAL STABILITY WAS EVALUATED AT THE CRITICAL WALL LOCATION.

**SOIL PARAMETERS**

| SOIL DESCRIPTION                                           | FRICITION ANGLE (DEGREES) | COHESION (PSF) | UNIT WEIGHT (PCF) |
|------------------------------------------------------------|---------------------------|----------------|-------------------|
| GRANULAR BACKFILL BEHIND THE WALL IN THE REINFORCING ZONE  | 30                        | 0              | 120               |
| FILL BEHIND THE REINFORCING ZONE                           | 31                        | 0              | 120               |
| SAND, BROWN, FINE, SOME GRAVEL                             | 31                        | 0              | 125               |
| SAND AND GRAVEL, BROWN, FINE TO COARSE                     | 33                        | 0              | 125               |
| SAND, BROWN, FINE TO COARSE, LITTLE FINE TO MEDIUM GRAVEL  | 32                        | 0              | 125               |
| SAND, BROWN, FINE, SOME SILT                               | 28                        | 0              | 120               |
| SILT, GRAY, TRACE FINE SAND                                | 0                         | 3,500          | 125               |
| SAND, BROWN, FINE, LITTLE GRAVEL                           | 32                        | 0              | 120               |
| SAND AND GRAVEL, BROWN, FINE TO COARSE                     | 33                        | 0              | 120               |
| SAND, BROWN, FINE TO COARSE, SOME GRAVEL                   | 33                        | 0              | 120               |
| SAND, GRAY/BROWN, FINE, SOME SILT                          | 31                        | 0              | 115               |
| SILT, GRAY, TRACE FINE SAND                                | 0                         | 2,750          | 120               |
| SILT, DARK BROWN & GRAY, LITTLE CLAY, TRACE FINE SAND      | 0                         | 4,500          | 125               |
| SAND, BROWN, FINE TO COARSE, SOME SILT, LITTLE FINE GRAVEL | 33                        | 0              | 125               |
| SILT, GRAY, TRACE FINE SAND                                | 0                         | 4,500          | 130               |

**WALL GEOMETRY TABLE**

| WALL STATION | R STATION   | OFFSET TO F.F. WALL | TOP OF WALL EL. | FINISHED GRADE EL. |
|--------------|-------------|---------------------|-----------------|--------------------|
| 00+00.00     | A 144+75.00 | 46.33' LT           | 842.70          | 841.53             |
| 00+25.00     | A 145+00.00 | 46.33' LT           | 845.45          | 842.03             |
| 00+40.00     | A 145+15.00 | 46.33' LT           | 847.10          | 842.33             |
| 00+50.00     | A 145+25.00 | 46.33' LT           | 847.24          | 842.56             |
| 00+75.00     | A 145+50.00 | 46.33' LT           | 847.60          | 843.28             |
| 00+75.00     | A 145+50.00 | 46.33' LT           | 847.60          | 840.68             |
| 01+00.00     | A 145+75.00 | 46.33' LT           | 851.43          | 841.46             |
| 01+20.00     | A 145+95.00 | 46.33' LT           | 854.50          | 842.04             |
| 01+25.00     | A 146+00.00 | 46.33' LT           | 854.50          | 842.18             |
| 01+50.00     | A 146+25.00 | 46.33' LT           | 854.50          | 842.91             |
| 01+75.55     | A 146+50.00 | 46.41' LT           | 852.00          | 843.63             |
| 02+01.73     | A 146+75.00 | 47.04' LT           | 849.50          | 844.35             |
| 02+15.43     | A 146+88.07 | 47.64' LT           | 848.12          | 844.75             |
| 02+27.95     | A 147+00.00 | 48.26' LT           | 847.00          | 845.13             |

Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1418  
 July 2, 2018

Project ID: 2788-00-71  
 Division 6 - Waukesha Bypass

| STATION | Distance | AREA (SF) |      |                |                 | Incremental Vol (CY) (Unadjusted) |     |      |                | Cumulative Vol (CY) |                  |     |      | Mass Ordinate |                |                 |                  |        |
|---------|----------|-----------|------|----------------|-----------------|-----------------------------------|-----|------|----------------|---------------------|------------------|-----|------|---------------|----------------|-----------------|------------------|--------|
|         |          | Cut       | Fill | EBS Excavation | Rock Excavation | Marsh Excavation                  | Cut | Fill | EBS Excavation | Rock Excavation     | Marsh Excavation | Cut | Fill |               | EBS Excavation | Rock Excavation | Marsh Excavation |        |
| 174+00  | AH       | 37        | 45   | 82             | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | Note 8 |
| 174+50  |          | 32        | 188  | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | -152   |
| 175+00  |          | 7         | 147  | 162            | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | -425   |
| 175+50  |          | 0         | 95   | 30             | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | -642   |
| 176+00  |          | 0         | 333  | 129            | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | -1,038 |
| 176+50  | BK       | 0         | 0    | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | -1,347 |
| 219+00  | AH       | 62        | 0    | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | -1,347 |
| 219+50  |          | 191       | 0    | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | -1,112 |
| 220+00  |          | 237       | 0    | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | -716   |
| 220+50  |          | 490       | 0    | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | -43    |
| 221+00  |          | 377       | 0    | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | 760    |
| 221+50  |          | 351       | 0    | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | 1,434  |
| 222+00  |          | 195       | 0    | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | 1,939  |
| 222+50  |          | 124       | 0    | 2              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | 2,233  |
| 223+00  |          | 72        | 0    | 5              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | 2,414  |
| 223+50  |          | 20        | 0    | 12             | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | 2,499  |
| 224+00  | BK       | 0         | 0    | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | 2,517  |
| 224+50  |          | 0         | 0    | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | 2,517  |
| 225+00  |          | 0         | 0    | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | 2,517  |
| 225+50  |          | 0         | 0    | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | 2,517  |
| 226+00  |          | 0         | 0    | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | 2,517  |
| 226+50  |          | 0         | 0    | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | 2,517  |
| 227+00  |          | 0         | 0    | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | 2,517  |
| 227+46  |          | 0         | 0    | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | 2,517  |
| 227+50  | 4        | 0         | 0    | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | 2,517  |
| 228+00  |          | 0         | 0    | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | 2,517  |
| 228+50  |          | 0         | 0    | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | 2,517  |
| 229+00  |          | 0         | 0    | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | 2,517  |
| 229+50  |          | 0         | 0    | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | 2,517  |
| 230+00  |          | 0         | 0    | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | 2,517  |
| 230+50  |          | 0         | 0    | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | 2,517  |
| 231+00  |          | 0         | 0    | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | 2,517  |
| 231+50  |          | 0         | 0    | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | 2,517  |
| 232+00  |          | 0         | 0    | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | 2,517  |
| 232+50  |          | 0         | 0    | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | 2,517  |
| 233+00  |          | 0         | 0    | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | 2,517  |
| 233+50  |          | 0         | 0    | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | 2,517  |
| 234+00  |          | 0         | 0    | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | 2,517  |
| 234+50  |          | 0         | 0    | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | 2,517  |
| 235+00  |          | 0         | 0    | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | 2,517  |
| 235+50  |          | 0         | 0    | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | 2,517  |
| 236+00  |          | 0         | 0    | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | 2,517  |
| 236+50  |          | 0         | 0    | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | 2,517  |
| 237+00  |          | 0         | 0    | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | 2,517  |
| 237+50  |          | 0         | 0    | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | 2,517  |
| 238+00  |          | 0         | 0    | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | 2,517  |
| 238+50  |          | 0         | 0    | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | 2,517  |
| 239+00  |          | 0         | 0    | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | 2,517  |
| 239+50  |          | 120       | 3    | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 0   | 0    | 0             | 0              | 0               | 0                | 2,626  |

PROJECT NO: 2788-00-71  
 HWY: WAUKESHA BYPASS COUNTY: WAUKESHA  
 EARTHWORK DATA  
 PLOT NAME: 27880001.dwg PLOT SCALE: 1:100000:1.000000  
 PLOT DATE: 6/26/2018 10:37 PLOT BY: tonner  
 SHEET NO: 1418  
 WISDOT CADDS SHEET 42

| STATION | Distance | AREA (SF) |      |                |                 |                  |   | Incremental Vol (CY) (Unadjusted) |      |                |                 |                  |   | Cumulative Vol (CY) |       |                |                 |                  |       | Mass Ordinate<br>Note 8 |
|---------|----------|-----------|------|----------------|-----------------|------------------|---|-----------------------------------|------|----------------|-----------------|------------------|---|---------------------|-------|----------------|-----------------|------------------|-------|-------------------------|
|         |          | Cut       | Fill | EBS Excavation | Rock Excavation | Marsh Excavation |   | Cut                               | Fill | EBS Excavation | Rock Excavation | Marsh Excavation |   | Cut                 | Fill  | EBS Excavation | Rock Excavation | Marsh Excavation |       |                         |
| 240+00  | 50       | 170       | 0    | 0              | 0               | 0                | 0 | 268                               | 2    | 0              | 0               | 0                | 0 | 4,351               | 1,459 | 707            | 0               | 0                | 2,892 |                         |
| 240+50  | 50       | 93        | 0    | 0              | 0               | 0                | 0 | 243                               | 0    | 0              | 0               | 0                | 0 | 4,594               | 1,459 | 707            | 0               | 0                | 3,135 |                         |
| 241+00  | 50       | 34        | 29   | 0              | 0               | 0                | 0 | 117                               | 27   | 0              | 0               | 0                | 0 | 4,711               | 1,485 | 707            | 0               | 0                | 3,225 |                         |
| 241+50  | 50       | 29        | 17   | 0              | 0               | 0                | 0 | 58                                | 43   | 0              | 0               | 0                | 0 | 4,769               | 1,528 | 707            | 0               | 0                | 3,241 |                         |
| 242+00  | 50       | 62        | 0    | 0              | 0               | 0                | 0 | 85                                | 16   | 0              | 0               | 0                | 0 | 4,854               | 1,544 | 707            | 0               | 0                | 3,310 |                         |
| 242+50  | 50       | 17        | 1    | 0              | 0               | 0                | 0 | 74                                | 1    | 0              | 0               | 0                | 0 | 4,828               | 1,544 | 707            | 0               | 0                | 3,383 |                         |
| 243+00  | 50       | 0         | 45   | 0              | 0               | 0                | 0 | 16                                | 42   | 0              | 0               | 0                | 0 | 4,944               | 1,586 | 707            | 0               | 0                | 3,357 |                         |
| 243+50  | 50       | 0         | 0    | 0              | 0               | 0                | 0 | 0                                 | 41   | 0              | 0               | 0                | 0 | 4,944               | 1,628 | 707            | 0               | 0                | 3,316 |                         |
| 244+00  | 50       | 0         | 0    | 0              | 0               | 0                | 0 | 0                                 | 0    | 0              | 0               | 0                | 0 | 4,944               | 1,628 | 707            | 0               | 0                | 3,316 |                         |
| 244+50  | 50       | 0         | 0    | 0              | 0               | 0                | 0 | 0                                 | 0    | 0              | 0               | 0                | 0 | 4,944               | 1,628 | 707            | 0               | 0                | 3,316 |                         |
| 245+00  | 50       | 0         | 0    | 0              | 0               | 0                | 0 | 0                                 | 0    | 0              | 0               | 0                | 0 | 4,944               | 1,628 | 707            | 0               | 0                | 3,316 |                         |
| 245+50  | 50       | 0         | 0    | 0              | 0               | 0                | 0 | 0                                 | 0    | 0              | 0               | 0                | 0 | 4,944               | 1,628 | 707            | 0               | 0                | 3,316 |                         |
| 246+00  | 50       | 0         | 0    | 0              | 0               | 0                | 0 | 0                                 | 0    | 0              | 0               | 0                | 0 | 4,944               | 1,628 | 707            | 0               | 0                | 3,316 |                         |
| 246+50  | 50       | 0         | 0    | 0              | 0               | 0                | 0 | 0                                 | 0    | 0              | 0               | 0                | 0 | 4,944               | 1,628 | 707            | 0               | 0                | 3,316 |                         |
| 247+00  | 50       | 0         | 0    | 0              | 0               | 0                | 0 | 0                                 | 0    | 0              | 0               | 0                | 0 | 4,944               | 1,628 | 707            | 0               | 0                | 3,316 |                         |
| 247+50  | 50       | 0         | 0    | 0              | 0               | 0                | 0 | 0                                 | 0    | 0              | 0               | 0                | 0 | 4,944               | 1,628 | 707            | 0               | 0                | 3,316 |                         |
| 248+00  | 50       | 0         | 0    | 0              | 0               | 0                | 0 | 0                                 | 0    | 0              | 0               | 0                | 0 | 4,944               | 1,628 | 707            | 0               | 0                | 3,316 |                         |
| 248+50  | 50       | 0         | 0    | 0              | 0               | 0                | 0 | 0                                 | 0    | 0              | 0               | 0                | 0 | 4,944               | 1,628 | 707            | 0               | 0                | 3,316 |                         |
| 249+00  | 50       | 0         | 0    | 0              | 0               | 0                | 0 | 0                                 | 0    | 0              | 0               | 0                | 0 | 4,944               | 1,628 | 707            | 0               | 0                | 3,316 |                         |
| 249+50  | 50       | 0         | 0    | 0              | 0               | 0                | 0 | 0                                 | 0    | 0              | 0               | 0                | 0 | 4,944               | 1,628 | 707            | 0               | 0                | 3,316 |                         |
| 250+00  | 50       | 0         | 0    | 0              | 0               | 0                | 0 | 0                                 | 0    | 0              | 0               | 0                | 0 | 4,944               | 1,628 | 707            | 0               | 0                | 3,316 |                         |
| 250+50  | 50       | 0         | 0    | 0              | 0               | 0                | 0 | 0                                 | 0    | 0              | 0               | 0                | 0 | 4,944               | 1,628 | 707            | 0               | 0                | 3,316 |                         |
| 251+00  | 50       | 0         | 0    | 0              | 0               | 0                | 0 | 0                                 | 0    | 0              | 0               | 0                | 0 | 4,944               | 1,628 | 707            | 0               | 0                | 3,316 |                         |
| 251+50  | 50       | 0         | 0    | 0              | 0               | 0                | 0 | 0                                 | 0    | 0              | 0               | 0                | 0 | 4,944               | 1,628 | 707            | 0               | 0                | 3,316 |                         |
| 252+00  | 50       | 0         | 0    | 0              | 0               | 0                | 0 | 0                                 | 0    | 0              | 0               | 0                | 0 | 4,944               | 1,628 | 707            | 0               | 0                | 3,316 |                         |
| 252+50  | 50       | 0         | 0    | 0              | 0               | 0                | 0 | 0                                 | 0    | 0              | 0               | 0                | 0 | 4,944               | 1,628 | 707            | 0               | 0                | 3,316 |                         |
| 253+00  | 50       | 0         | 0    | 0              | 0               | 0                | 0 | 0                                 | 0    | 0              | 0               | 0                | 0 | 4,944               | 1,628 | 707            | 0               | 0                | 3,316 |                         |
| 253+50  | 50       | 0         | 0    | 0              | 0               | 0                | 0 | 0                                 | 0    | 0              | 0               | 0                | 0 | 4,944               | 1,628 | 707            | 0               | 0                | 3,316 |                         |
| 254+00  | 50       | 0         | 0    | 0              | 0               | 0                | 0 | 0                                 | 0    | 0              | 0               | 0                | 0 | 4,944               | 1,628 | 707            | 0               | 0                | 3,316 |                         |
| 254+50  | 50       | 0         | 0    | 0              | 0               | 0                | 0 | 0                                 | 0    | 0              | 0               | 0                | 0 | 4,944               | 1,628 | 707            | 0               | 0                | 3,316 |                         |
| 255+00  | 50       | 6         | 6    | 0              | 0               | 0                | 0 | 0                                 | 5    | 0              | 0               | 0                | 0 | 4,944               | 1,633 | 707            | 0               | 0                | 3,311 |                         |
| 255+50  | 50       | 56        | 0    | 0              | 0               | 0                | 0 | 52                                | 5    | 0              | 0               | 0                | 0 | 4,986               | 1,638 | 707            | 0               | 0                | 3,358 |                         |
| 256+00  | 50       | 0         | 0    | 0              | 0               | 0                | 0 | 0                                 | 0    | 0              | 0               | 0                | 0 | 5,048               | 1,638 | 707            | 0               | 0                | 3,410 |                         |
| 256+50  | 50       | 276       | 0    | 0              | 0               | 0                | 0 | 256                               | 0    | 0              | 0               | 0                | 0 | 5,304               | 1,638 | 707            | 0               | 0                | 3,666 |                         |
| 257+00  | 50       | 370       | 0    | 0              | 0               | 0                | 0 | 586                               | 0    | 0              | 0               | 0                | 0 | 5,902               | 1,638 | 707            | 0               | 0                | 4,284 |                         |
| 257+50  | 50       | 403       | 0    | 0              | 0               | 0                | 0 | 715                               | 0    | 0              | 0               | 0                | 0 | 6,617               | 1,638 | 707            | 0               | 0                | 4,979 |                         |
| 258+00  | 50       | 435       | 0    | 0              | 0               | 0                | 0 | 776                               | 0    | 0              | 0               | 0                | 0 | 7,393               | 1,638 | 707            | 0               | 0                | 5,754 |                         |
| 258+50  | 50       | 402       | 0    | 0              | 0               | 0                | 0 | 775                               | 0    | 0              | 0               | 0                | 0 | 8,167               | 1,638 | 707            | 0               | 0                | 6,529 |                         |
| 259+00  | 50       | 267       | 0    | 0              | 0               | 0                | 0 | 619                               | 0    | 0              | 0               | 0                | 0 | 8,786               | 1,638 | 707            | 5               | 0                | 7,153 |                         |
| 259+50  | 50       | 155       | 0    | 0              | 0               | 0                | 0 | 380                               | 0    | 0              | 0               | 0                | 0 | 9,177               | 1,638 | 707            | 14              | 0                | 7,552 |                         |
| 260+00  | 50       | 37        | 0    | 0              | 0               | 0                | 0 | 178                               | 0    | 0              | 0               | 0                | 0 | 9,354               | 1,638 | 707            | 23              | 0                | 7,739 |                         |
| 260+50  | 50       | 9         | 0    | 0              | 0               | 0                | 0 | 34                                | 0    | 0              | 0               | 0                | 0 | 9,389               | 1,638 | 707            | 28              | 0                | 7,778 |                         |
| 261+00  | 50       | 9         | 0    | 0              | 0               | 0                | 0 | 9                                 | 0    | 0              | 0               | 0                | 0 | 9,397               | 1,638 | 707            | 28              | 0                | 7,787 |                         |
| 261+50  | 50       | 11        | 0    | 0              | 0               | 0                | 0 | 18                                | 0    | 0              | 0               | 0                | 0 | 9,415               | 1,638 | 707            | 28              | 0                | 7,805 |                         |
| 262+00  | 50       | 15        | 0    | 0              | 0               | 0                | 0 | 23                                | 0    | 0              | 0               | 0                | 0 | 9,439               | 1,638 | 707            | 32              | 0                | 7,833 |                         |
| 262+50  | 50       | 45        | 0    | 0              | 0               | 0                | 0 | 55                                | 0    | 0              | 0               | 0                | 0 | 9,494               | 1,638 | 707            | 42              | 0                | 7,898 |                         |
| 263+00  | 50       | 61        | 0    | 0              | 0               | 0                | 0 | 98                                | 0    | 0              | 0               | 0                | 0 | 9,593               | 1,638 | 707            | 51              | 0                | 8,006 |                         |
| 263+50  | 50       | 94        | 0    | 0              | 0               | 0                | 0 | 143                               | 0    | 0              | 0               | 0                | 0 | 9,736               | 1,638 | 707            | 60              | 0                | 8,158 |                         |
| 263+95  | 45       | 89        | 0    | 0              | 0               | 0                | 0 | 152                               | 0    | 0              | 0               | 0                | 0 | 9,889               | 1,638 | 707            | 69              | 0                | 8,319 |                         |
| 264+00  | 5        | 88        | 0    | 0              | 0               | 0                | 0 | 16                                | 0    | 0              | 0               | 0                | 0 | 9,905               | 1,638 | 707            | 69              | 0                | 8,336 |                         |
| 264+50  | 50       | 59        | 0    | 0              | 0               | 0                | 0 | 135                               | 0    | 0              | 0               | 0                | 0 | 10,040              | 1,638 | 707            | 79              | 0                | 8,481 |                         |

Project ID: 2788-00-71

Division 6 - Waukesha Bypass

| STATION   | Distance | AREA (SF) |       |                |                 | Incremental Vol (CY) (Unadjusted) |     |      |                | Cumulative Vol (CY) |                  |        |       | Mass Ordinate | Note 8 |                |
|-----------|----------|-----------|-------|----------------|-----------------|-----------------------------------|-----|------|----------------|---------------------|------------------|--------|-------|---------------|--------|----------------|
|           |          | Cut       | Fill  | EBS Excavation | Rock Excavation | Marsh Excavation                  | Cut | Fill | EBS Excavation | Rock Excavation     | Marsh Excavation | Cut    | Fill  |               |        | EBS Excavation |
| 265+00    | 50       | 0         | 68    | 0              | 0               | 0                                 | 0   | 0    | 5              | 0                   | 63               | 1,701  | 707   | 83            | 0      | 8,477          |
| 265+50    | 50       | 0         | 174   | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 223              | 1,924  | 707   | 83            | 0      | 8,254          |
| 266+00    | 50       | 0         | 349   | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 484              | 2,408  | 707   | 83            | 0      | 7,770          |
| 266+14    | 14       | 0         | 372   | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 187              | 2,595  | 707   | 83            | 0      | 7,583          |
| 266+50    | 36       | 0         | 441   | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 542              | 3,137  | 707   | 83            | 0      | 7,041          |
| 267+00    | 50       | 0         | 456   | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 831              | 3,968  | 707   | 83            | 0      | 6,210          |
| 267+50    | 50       | 0         | 444   | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 833              | 4,801  | 707   | 83            | 0      | 5,377          |
| 268+00    | 50       | 0         | 391   | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 773              | 5,574  | 707   | 83            | 0      | 4,604          |
| 268+50    | 50       | 0         | 324   | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 661              | 6,235  | 707   | 83            | 0      | 3,942          |
| 269+00    | 50       | 0         | 168   | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 455              | 6,691  | 707   | 83            | 0      | 3,487          |
| 269+50    | 50       | 0         | 144   | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 289              | 6,979  | 707   | 83            | 0      | 3,199          |
| 270+00    | 50       | 0         | 111   | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 236              | 7,216  | 707   | 83            | 0      | 2,962          |
| 270+01    | 1        | 0         | 110   | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 4                | 7,220  | 707   | 83            | 0      | 2,958          |
| 270+36    | 35       | 0         | 60    | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 110              | 7,330  | 707   | 83            | 0      | 2,848          |
| 270+50    | 14       | 0         | 31    | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 24               | 7,354  | 707   | 83            | 0      | 2,824          |
| 271+00    | 50       | 0         | 8     | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 36               | 7,390  | 707   | 83            | 0      | 2,788          |
| 271+05    | 5        | 0         | 0     | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 1                | 7,391  | 707   | 83            | 0      | 2,787          |
| 271+50    | 45       | 0         | 0     | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 7,391  | 707   | 83            | 0      | 2,787          |
| 272+00    | 50       | 0         | 0     | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 7,391  | 707   | 83            | 0      | 2,787          |
| 272+50    | 50       | 0         | 0     | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 7,391  | 707   | 83            | 0      | 2,787          |
| 273+00    | 50       | 0         | 0     | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 7,391  | 707   | 83            | 0      | 2,787          |
| 273+50    | 50       | 0         | 0     | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 7,391  | 707   | 83            | 0      | 2,787          |
| 273+75 BK | 25       | 0         | 0     | 0              | 0               | 0                                 | 0   | 0    | 0              | 0                   | 0                | 7,391  | 707   | 83            | 0      | 2,787          |
|           |          | 10,095    | 7,391 | 707            | 83              | 0                                 | 0   | 0    | 0              | 0                   | 0                | 10,095 | 7,391 | 707           | 83     | 0              |

Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1420  
 July 2, 2018

PROJECT NO: 2788-00-71

HWY: WAUKESHA BYPASS

COUNTY: WAUKESHA

EARTHWORK DATA

SHEET NO: 1420

E

FILE NAME : F:\BM\3324\US18 Bypass\6\_Design\03\_Road\cds\by\_09\01\_sw.ppt

PLOT BY : toner

PLOT NAME : by09001.sw

PLOT SCALE : 1.0000001.00000

WISDOT / CADDS SHEET 42

Addendum No. 01  
 ID 2788-00-71  
 Added Sheet 1432A  
 July 2, 2018

Project ID: 2788-00-71  
 Division 7 - MacArthur Road

| STATION | Distance | AREA (SF) |      |                   |                     | Incremental Vol (CY) (Unadjusted) |      |                   |                     | Cumulative Vol (CY) |      |                   |                     | Marsh<br>Ordinate<br>Note 8 |
|---------|----------|-----------|------|-------------------|---------------------|-----------------------------------|------|-------------------|---------------------|---------------------|------|-------------------|---------------------|-----------------------------|
|         |          | Cut       | Fill | EBS<br>Excavation | Marsh<br>Excavation | Cut                               | Fill | EBS<br>Excavation | Marsh<br>Excavation | Cut                 | Fill | EBS<br>Excavation | Marsh<br>Excavation |                             |
| 56+30   | AH       | 0         | 0    | 0                 | 0                   | 0                                 | 0    | 0                 | 0                   | 0                   | 0    | 0                 | 0                   | 0                           |
| 56+50   |          | 192       | 0    | 0                 | 0                   | 71                                | 0    | 0                 | 0                   | 71                  | 0    | 0                 | 0                   | 71                          |
| 56+61   |          | 227       | --   | 0                 | 0                   | --                                | --   | --                | 0                   | --                  | --   | --                | 0                   | --                          |
| 56+86   |          | 91        | 4    | 0                 | 0                   | --                                | --   | --                | 0                   | --                  | --   | --                | 0                   | --                          |
| 57+00   |          | 61        | 0    | 0                 | 0                   | 264                               | 3    | 0                 | 0                   | 335                 | 3    | 0                 | 0                   | 332                         |
| 57+50   |          | 34        | 0    | 0                 | 0                   | --                                | --   | --                | 0                   | --                  | --   | --                | 0                   | --                          |
| 57+60   | BK       | 35        | 1    | 0                 | 0                   | 96                                | 1    | 0                 | 0                   | 431                 | 3    | 0                 | 0                   | 427                         |
|         |          |           |      |                   |                     | 431                               | 3    | 0                 | 0                   |                     |      |                   |                     |                             |



Project ID: 2788-00-71  
Division 7 - Kisdon Hill Drive (3-2)

| STATION | Distance | AREA (SF) |       |                |                 |                  | Incremental Vol.(CY) (Unadjusted) |      |                |                 |                  | Cumulative Vol.(CY) |      |                |                 |                  | Mass Ordinate<br>Note 8 |       |
|---------|----------|-----------|-------|----------------|-----------------|------------------|-----------------------------------|------|----------------|-----------------|------------------|---------------------|------|----------------|-----------------|------------------|-------------------------|-------|
|         |          | Cut       | Fill  | EBS Excavation | Rock Excavation | Marsh Excavation | Cut                               | Fill | EBS Excavation | Rock Excavation | Marsh Excavation | Cut                 | Fill | EBS Excavation | Rock Excavation | Marsh Excavation |                         |       |
| 50+95   | AH       | 209       | 0     | 0              | 0               | 0                | 0                                 | 0    | 0              | 0               | 0                | 0                   | 0    | 0              | 0               | 0                | 0                       | 0     |
| 51+00   | 5        | 223       | 0     | 0              | 0               | 3                | 0                                 | 0    | 0              | 0               | 0                | 0                   | 0    | 0              | 0               | 0                | 0                       | 41    |
| 51+50   | 50       | 284       | 0     | 0              | 0               | 3                | 0                                 | 0    | 0              | 0               | 0                | 0                   | 0    | 0              | 0               | 0                | 0                       | 520   |
| 52+00   | 50       | 240       | 0     | 0              | 0               | 0                | 0                                 | 0    | 0              | 0               | 0                | 0                   | 0    | 0              | 0               | 0                | 0                       | 1,008 |
| 52+50   | 50       | 110       | 0     | 0              | 0               | 0                | 0                                 | 0    | 0              | 0               | 0                | 0                   | 0    | 0              | 0               | 0                | 0                       | 1,332 |
| 53+00   | 50       | 59        | 0     | 0              | 0               | 0                | 0                                 | 0    | 0              | 0               | 0                | 0                   | 0    | 0              | 0               | 0                | 0                       | 1,488 |
| 53+50   | 50       | 53        | 0     | 0              | 0               | 0                | 0                                 | 0    | 0              | 0               | 0                | 0                   | 0    | 0              | 0               | 0                | 0                       | 1,591 |
| 53+75   | BK       | 46        | 0     | 0              | 0               | 0                | 0                                 | 0    | 0              | 0               | 0                | 0                   | 0    | 0              | 0               | 0                | 0                       | 1,636 |
|         |          |           | 1,622 | 0              | 0               | 0                | 0                                 | 0    | 0              | 0               | 0                | 0                   | 0    | 0              | 0               | 0                | 0                       |       |

Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1436  
July 2, 2018

Project ID: 2788-00-71  
Division 7 - Kisdon Hill Drive (3-3)

| STATION | Distance | AREA (SF) |      |                |                 |                  | Incremental Vol.(CY) (Unadjusted) |      |                |                 |                  | Cumulative Vol.(CY) |      |                |                 |                  | Mass Ordinate<br>Note 8 |      |
|---------|----------|-----------|------|----------------|-----------------|------------------|-----------------------------------|------|----------------|-----------------|------------------|---------------------|------|----------------|-----------------|------------------|-------------------------|------|
|         |          | Cut       | Fill | EBS Excavation | Rock Excavation | Marsh Excavation | Cut                               | Fill | EBS Excavation | Rock Excavation | Marsh Excavation | Cut                 | Fill | EBS Excavation | Rock Excavation | Marsh Excavation |                         |      |
| 50+95   | AH       | 47        | 13   | 0              | 0               | 10               | 0                                 | 0    | 0              | 0               | 0                | 0                   | 0    | 0              | 0               | 0                | 0                       | 0    |
| 51+00   | 5        | 32        | 16   | 0              | 0               | 12               | 0                                 | 0    | 0              | 0               | 0                | 0                   | 0    | 0              | 0               | 0                | 0                       | 5    |
| 51+50   | 50       | 2         | 61   | 0              | 0               | 0                | 0                                 | 0    | 0              | 0               | 0                | 0                   | 0    | 0              | 0               | 0                | 0                       | -35  |
| 52+00   | 50       | 1         | 81   | 0              | 0               | 0                | 0                                 | 0    | 0              | 0               | 0                | 0                   | 0    | 0              | 0               | 0                | 0                       | -164 |
| 52+50   | 50       | 2         | 35   | 0              | 0               | 0                | 0                                 | 0    | 0              | 0               | 0                | 0                   | 0    | 0              | 0               | 0                | 0                       | -269 |
| 53+00   | 50       | 7         | 18   | 0              | 0               | 0                | 0                                 | 0    | 0              | 0               | 0                | 0                   | 0    | 0              | 0               | 0                | 0                       | -309 |
| 53+50   | 50       | 6         | 10   | 0              | 0               | 0                | 0                                 | 0    | 0              | 0               | 0                | 0                   | 0    | 0              | 0               | 0                | 0                       | -322 |
| 53+75   | BK       | 3         | 16   | 0              | 0               | 0                | 0                                 | 0    | 0              | 0               | 0                | 0                   | 0    | 0              | 0               | 0                | 0                       | -330 |
|         |          |           | 67   | 388            | 0               | 0                | 0                                 | 0    | 0              | 0               | 0                | 0                   | 0    | 0              | 0               | 0                | 0                       |      |

Project ID: 2788-00-71  
Division 2 - Genesee Driveway Station 47+25 LT

| STATION | Distance | AREA (SF) |      |                |                  | Incremental Vol (CY) (Unadjusted) |      |                |                  | Cumulative Vol (CY) |      |                |                  | Mass Ordinate<br>Note 8 |
|---------|----------|-----------|------|----------------|------------------|-----------------------------------|------|----------------|------------------|---------------------|------|----------------|------------------|-------------------------|
|         |          | Cut       | Fill | EBS Excavation | Marsh Excavation | Cut                               | Fill | EBS Excavation | Marsh Excavation | Cut                 | Fill | EBS Excavation | Marsh Excavation |                         |
| 10+64   | AH       | 0         | 9    | 0              | 0                | 0                                 | 0    | 0              | 0                | 0                   | 0    | 0              | 0                | 0                       |
| 10+75   |          | 81        | 2    | 0              | 0                | 16                                | 2    | 0              | 0                | 16                  | 2    | 0              | 0                | 14                      |
| 11+00   |          | 131       | 1    | 0              | 0                | 98                                | 2    | 0              | 0                | 115                 | 4    | 0              | 0                | 111                     |
| 11+25   |          | 114       | 2    | 0              | 0                | 113                               | 2    | 0              | 0                | 228                 | 6    | 0              | 0                | 222                     |
| 11+50   |          | 97        | 3    | 0              | 0                | 98                                | 2    | 0              | 0                | 326                 | 8    | 0              | 0                | 318                     |
| 11+75   |          | 80        | 11   | 0              | 0                | 82                                | 6    | 0              | 0                | 408                 | 14   | 0              | 0                | 394                     |
| 12+00   |          | 38        | 28   | 0              | 0                | 54                                | 18   | 0              | 0                | 462                 | 32   | 0              | 0                | 429                     |
| 12+25   |          | 14        | 24   | 0              | 0                | 23                                | 24   | 0              | 0                | 485                 | 57   | 0              | 0                | 428                     |
| 12+50   |          | 10        | 27   | 0              | 0                | 11                                | 24   | 0              | 0                | 496                 | 80   | 0              | 0                | 415                     |
| 12+75   |          | 14        | 14   | 0              | 0                | 11                                | 19   | 0              | 0                | 507                 | 99   | 0              | 0                | 407                     |
| 12+88   | BK       | 18        | 17   | 0              | 0                | 8                                 | 8    | 0              | 0                | 515                 | 107  | 0              | 0                | 408                     |
|         |          |           |      |                |                  | 515                               | 107  | 0              | 0                |                     |      |                |                  |                         |

Project ID: 2788-00-71  
Division 2 - Driveway 208+25

| STATION | Distance | AREA (SF) |      |                |                  | Incremental Vol (CY) (Unadjusted) |      |                |                  | Cumulative Vol (CY) |      |                |                  | Mass Ordinate<br>Note 8 |
|---------|----------|-----------|------|----------------|------------------|-----------------------------------|------|----------------|------------------|---------------------|------|----------------|------------------|-------------------------|
|         |          | Cut       | Fill | EBS Excavation | Marsh Excavation | Cut                               | Fill | EBS Excavation | Marsh Excavation | Cut                 | Fill | EBS Excavation | Marsh Excavation |                         |
| 300+68  | AH       | 139       | 0    | 0              | 0                | 0                                 | 0    | 0              | 0                | 0                   | 0    | 0              | 0                | 0                       |
| 300+75  |          | 164       | 0    | 0              | 0                | 39                                | 0    | 0              | 0                | 39                  | 0    | 0              | 0                | 39                      |
| 301+00  |          | 116       | 0    | 0              | 0                | 129                               | 0    | 0              | 0                | 168                 | 0    | 0              | 0                | 168                     |
| 301+25  |          | 61        | 0    | 0              | 0                | 82                                | 0    | 0              | 0                | 250                 | 0    | 0              | 0                | 250                     |
| 301+50  |          | 16        | 0    | 0              | 0                | 35                                | 0    | 0              | 0                | 285                 | 0    | 0              | 0                | 285                     |
| 301+75  | BK       | 10        | 0    | 0              | 0                | 12                                | 0    | 0              | 0                | 297                 | 0    | 0              | 0                | 297                     |
|         |          |           |      |                |                  | 297                               | 0    | 0              | 0                |                     |      |                |                  |                         |

Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1440  
July 2, 2018

Project ID: 2788-00-71  
Division 2 - Driveway 222+76

| STATION | Distance | AREA (SF) |      |                |                  | Incremental Vol (CY) (Unadjusted) |       |                |                  | Cumulative Vol (CY) |       |                |                  | Mass Ordinate<br>Note 8 |
|---------|----------|-----------|------|----------------|------------------|-----------------------------------|-------|----------------|------------------|---------------------|-------|----------------|------------------|-------------------------|
|         |          | Cut       | Fill | EBS Excavation | Marsh Excavation | Cut                               | Fill  | EBS Excavation | Marsh Excavation | Cut                 | Fill  | EBS Excavation | Marsh Excavation |                         |
| 10+68   | AH       | 0         | 244  | 0              | 0                | 0                                 | 0     | 0              | 0                | 0                   | 0     | 0              | 0                | 0                       |
| 10+75   |          | 0         | 309  | 0              | 0                | 0                                 | 72    | 0              | 0                | 0                   | 72    | 0              | 0                | -72                     |
| 11+00   |          | 0         | 369  | 0              | 0                | 0                                 | 314   | 0              | 0                | 0                   | 385   | 0              | 0                | -385                    |
| 11+25   |          | 0         | 480  | 0              | 0                | 0                                 | 384   | 0              | 0                | 0                   | 769   | 0              | 0                | -769                    |
| 11+50   |          | 0         | 387  | 0              | 0                | 0                                 | 392   | 0              | 0                | 0                   | 1,161 | 0              | 0                | -1,161                  |
| 11+75   |          | 0         | 197  | 0              | 0                | 0                                 | 271   | 0              | 0                | 0                   | 1,432 | 0              | 0                | -1,432                  |
| 12+00   |          | 0         | 67   | 0              | 0                | 0                                 | 123   | 0              | 0                | 0                   | 1,554 | 0              | 0                | -1,554                  |
| 12+25   |          | 12        | 13   | 0              | 0                | 6                                 | 37    | 0              | 0                | 6                   | 1,592 | 0              | 0                | -1,586                  |
| 12+50   |          | 9         | 6    | 0              | 0                | 10                                | 9     | 0              | 0                | 15                  | 1,600 | 0              | 0                | -1,585                  |
| 12+77   | BK       | 0         | 16   | 0              | 0                | 5                                 | 11    | 0              | 0                | 20                  | 1,611 | 0              | 0                | -1,591                  |
|         |          |           |      |                |                  | 20                                | 1,611 | 0              | 0                |                     |       |                |                  |                         |

PROJECT NO: 2788-00-71

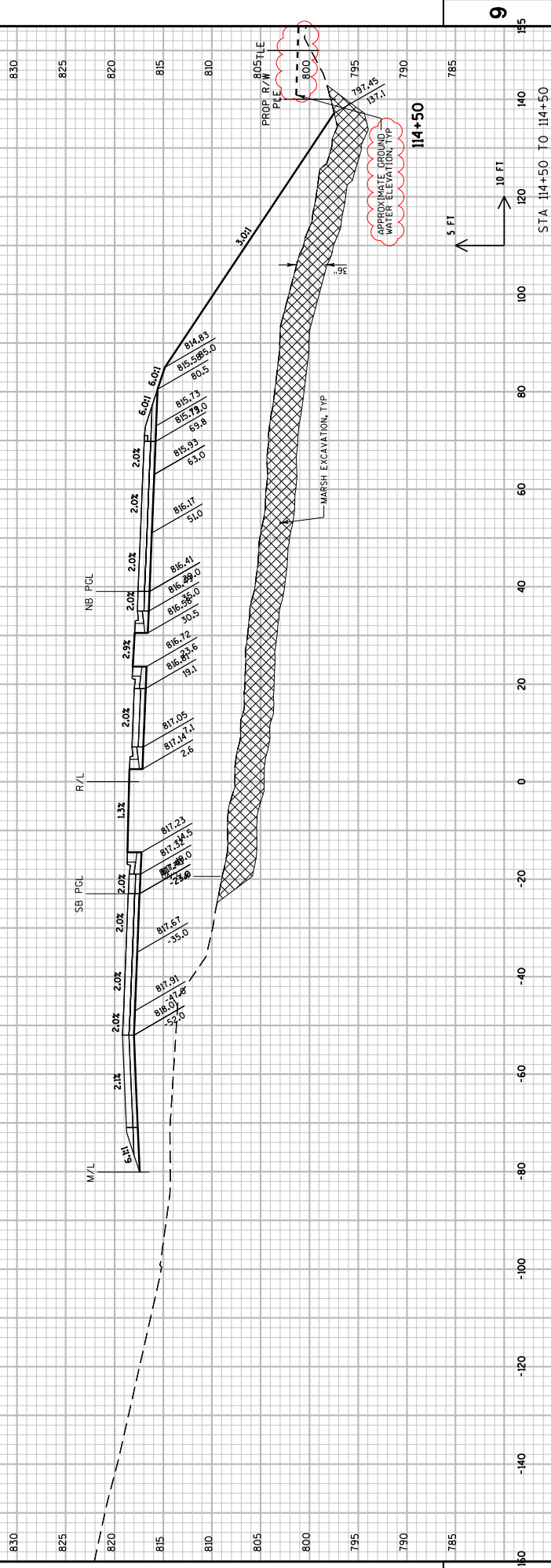
HWY: WAUKESHA BYPASS COUNTY: WAUKESHA

EARTHWORK DATA

SHEET NO: 1440

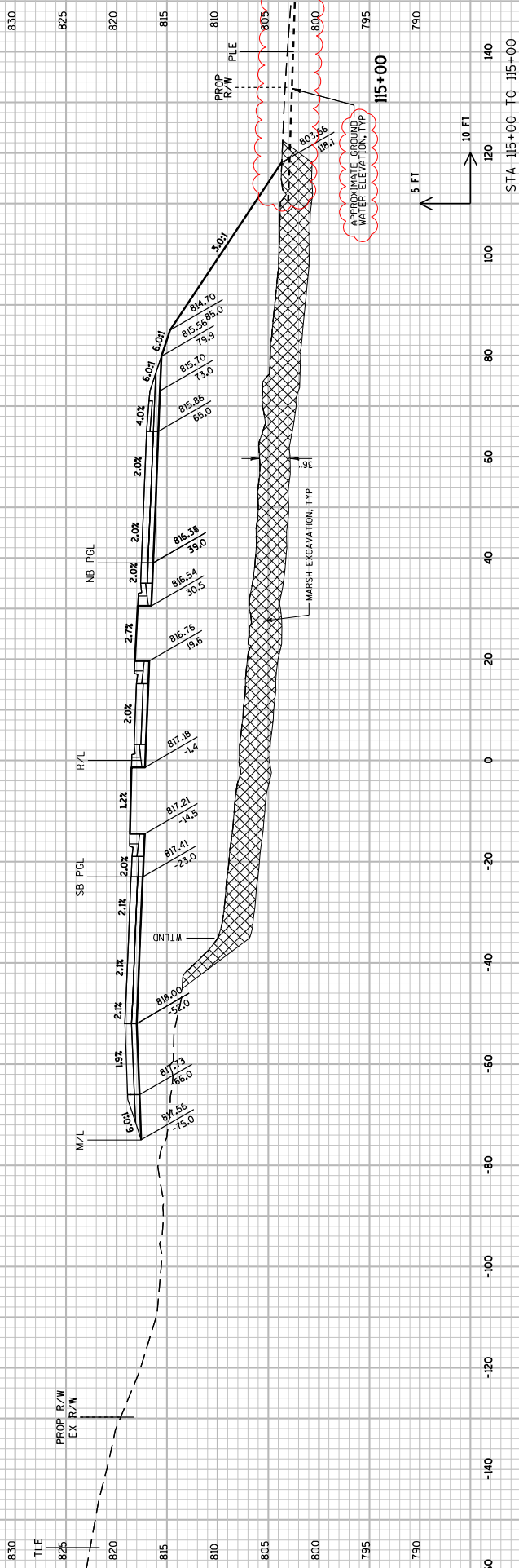
ALL WORK COMPLETED IN STAGE 2A

Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1462  
July 2, 2018



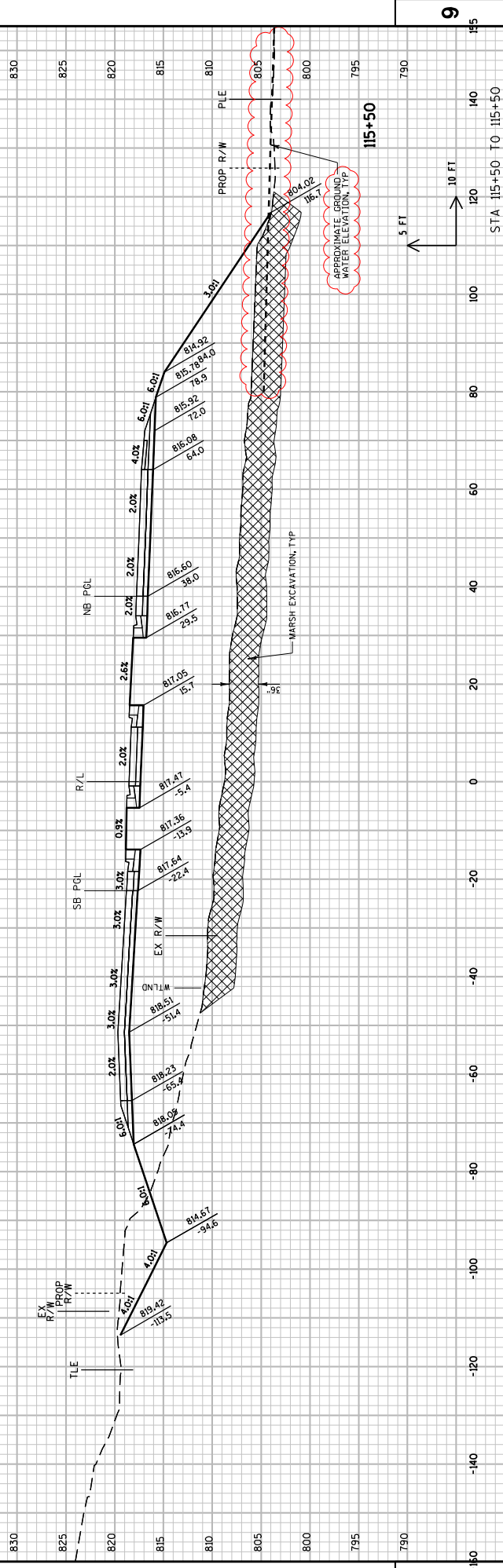
Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1463  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A



Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1464  
 July 2, 2018

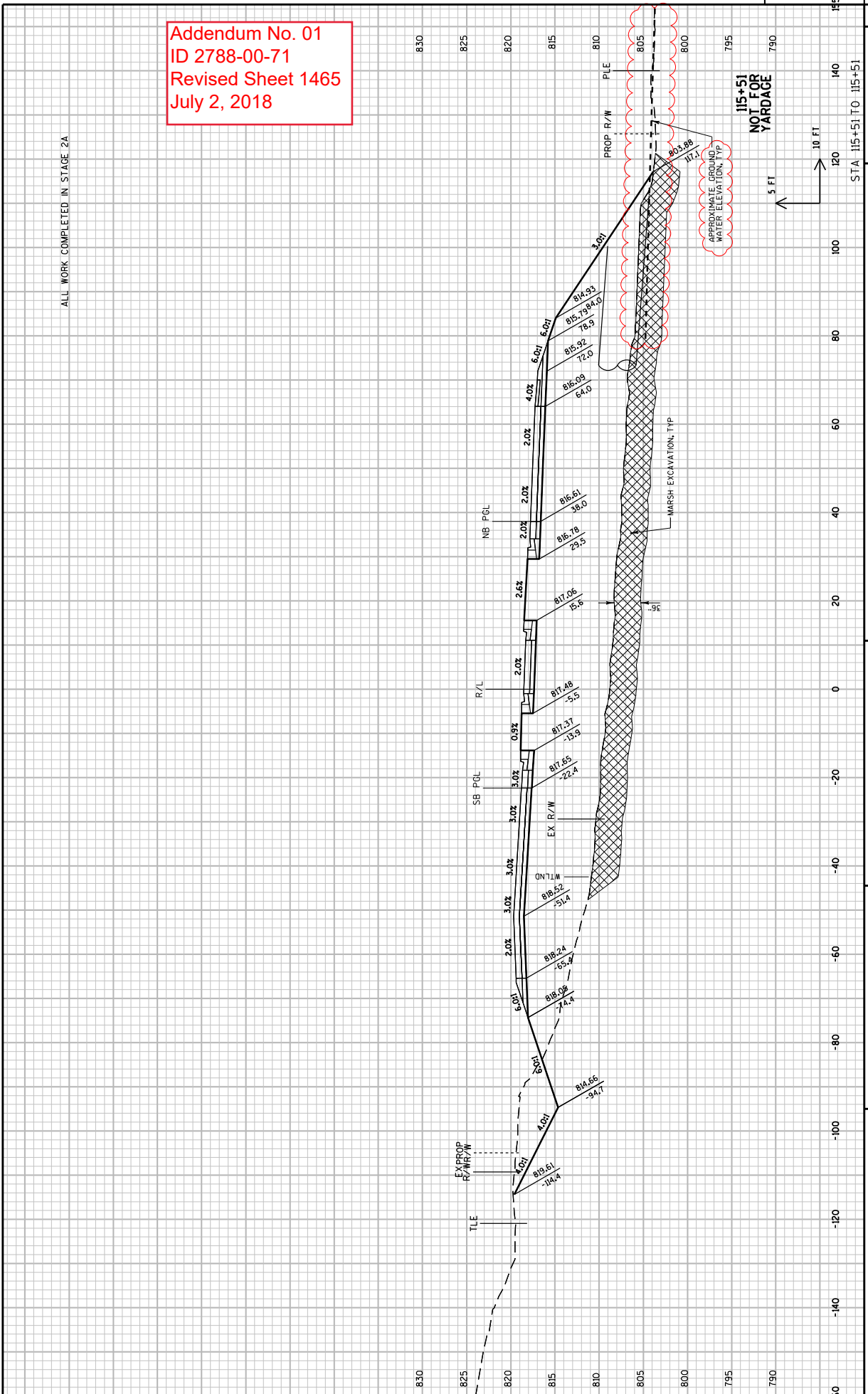
ALL WORK COMPLETED IN STAGE 2A



|                                                                 |                  |                                 |                          |
|-----------------------------------------------------------------|------------------|---------------------------------|--------------------------|
| PROJECT NO: 2788-00-71                                          | COUNTY: WAUKESHA | CROSS SECTIONS: WAUKESHA BYPASS | SHEET 1464               |
| FILE NAME : \\wplw00\jmgr\proj\59971\1\cadd\by_090201_xs_mb.dgn |                  |                                 | PLOT SCALE : 20:1        |
| PLOT DATE : 6/26/2018 10:24:36 AM                               |                  |                                 | PLOT BY : TFW.LP@wis.edu |
| PLOT NAME :                                                     |                  |                                 | WISDOT/CADD SHEET 21     |

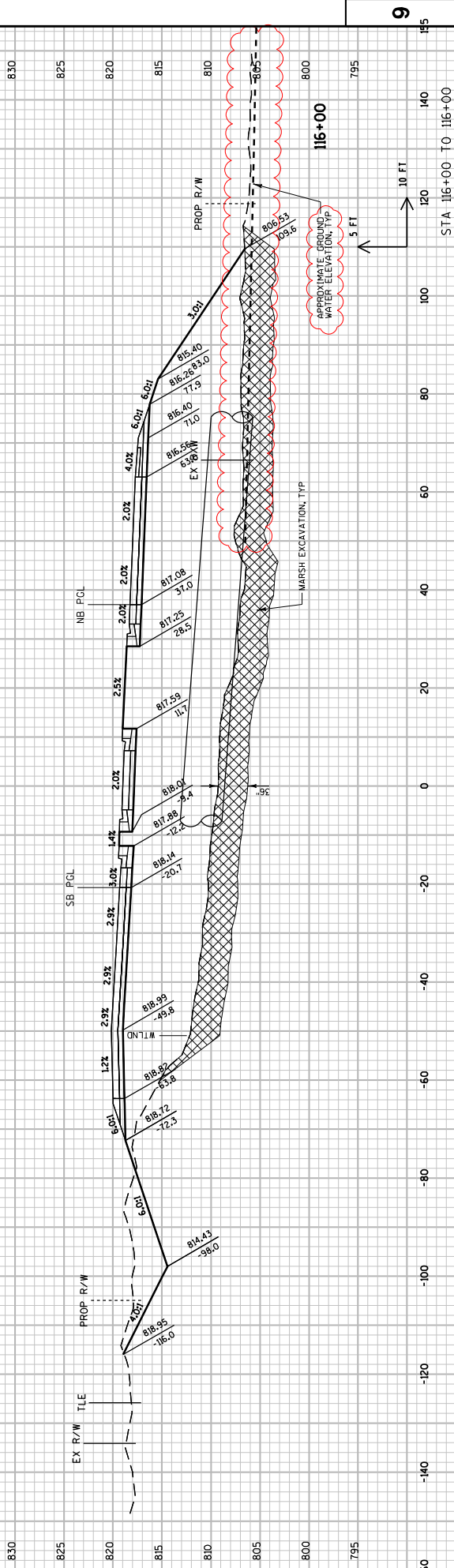
Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1465  
July 2, 2018

ALL WORK COMPLETED IN STAGE 2A



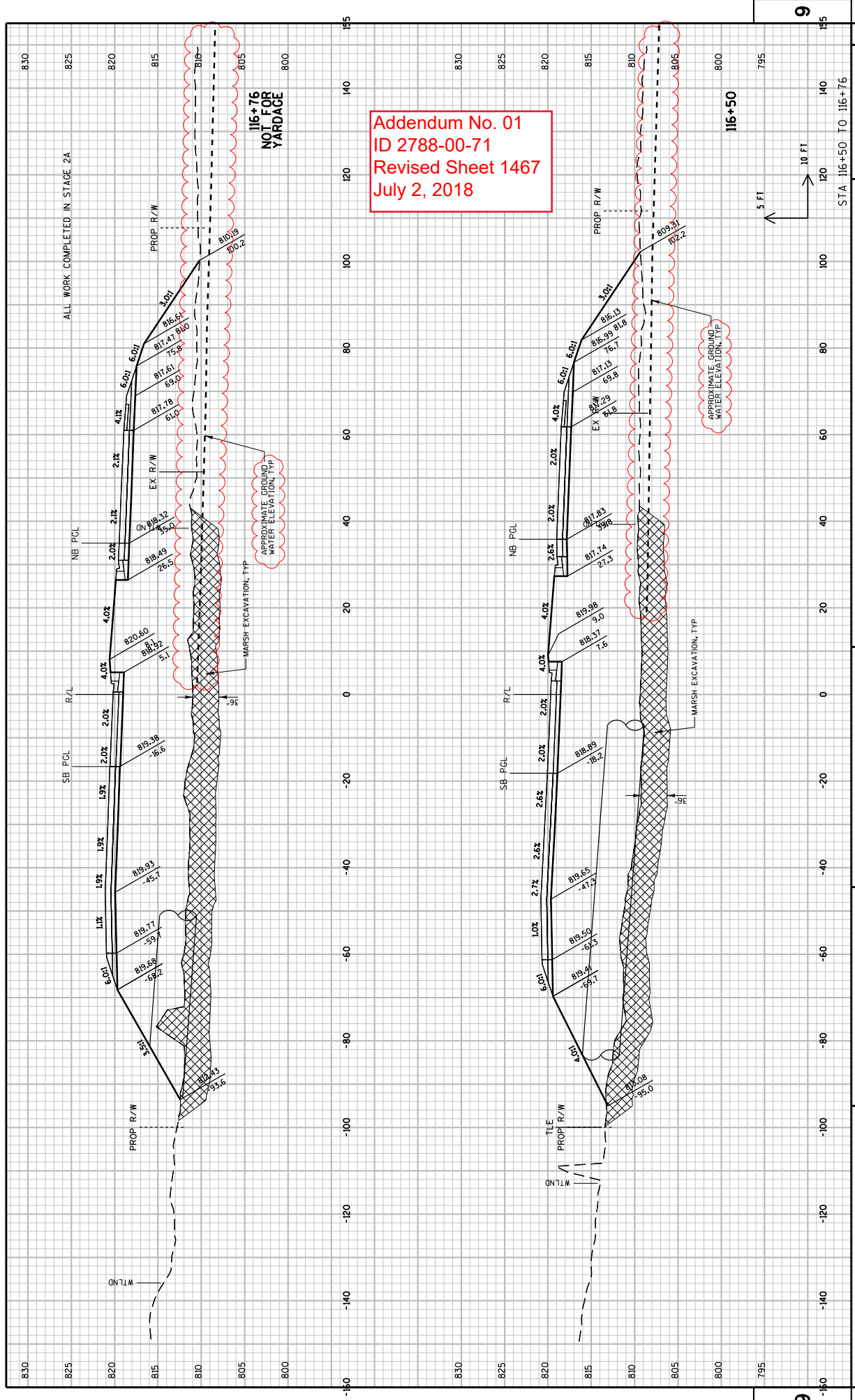
ALL WORK COMPLETED IN STAGE 2A

Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1466  
July 2, 2018



PROJECT NO: 2788-00-71  
 COUNTY: WAUKESHA  
 HWY: WAUKESHA BYPASS  
 CROSS SECTIONS: WAUKESHA BYPASS  
 SHEET 1466  
 WISDOT/CADD SHEET 21

FILE NAME : \\nw11w00\lmp\proj\59971\1\cadd\sy\_090201\_xs\_mb.dgn  
 PLOT BY : fww.lrp@wi  
 PLOT DATE : 6/26/2018 10:24:38 AM  
 PLOT SCALE : 20:1



Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1467  
 July 2, 2018

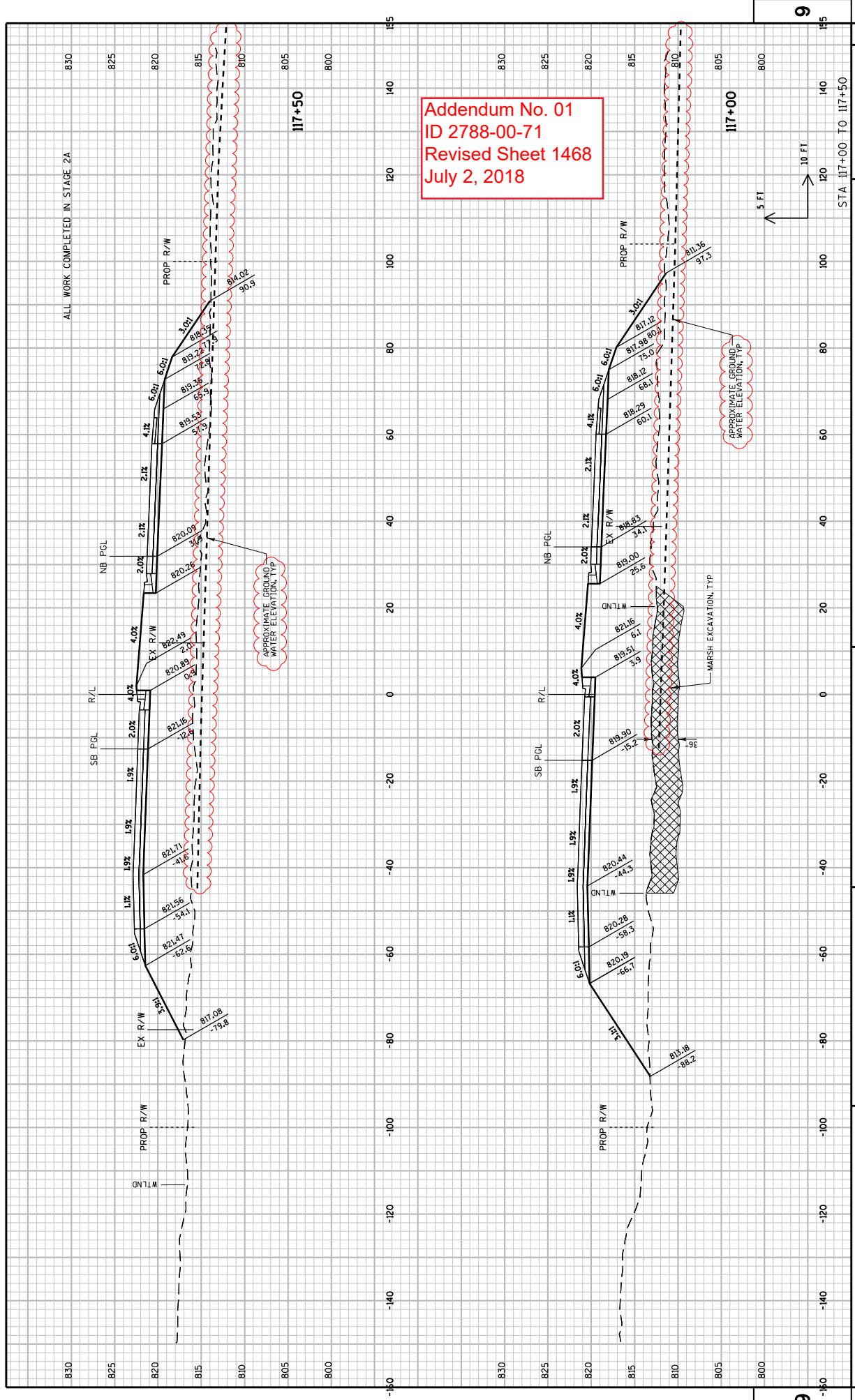
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PROJECT NO: 2788-00-71 COUNTY: WAUKESHA HWY: WAUKESHA BYPASS CROSS SECTIONS: WAUKESHA BYPASS SHEET 1467 E

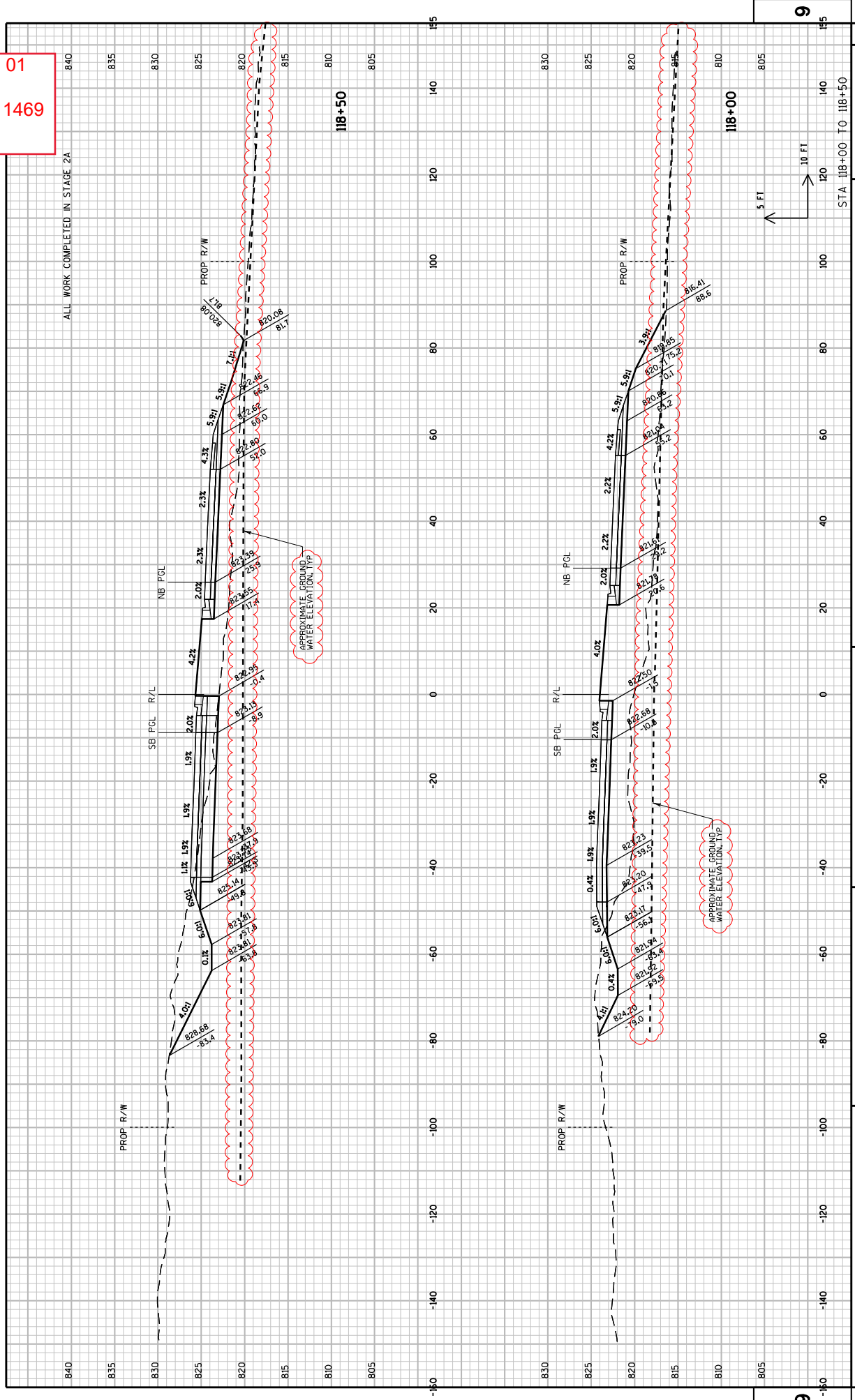
FILE NAME : \\nw11w00\jngpr\proj\59971\1\cadd\by\_090201\_xs\_mb.dgn PLOT DATE : 6/26/2018 10:24:39 AM PLOT BY : fhw11p@iwi PLOT SCALE : 20:1 WISDOT/CADD SHEET 21





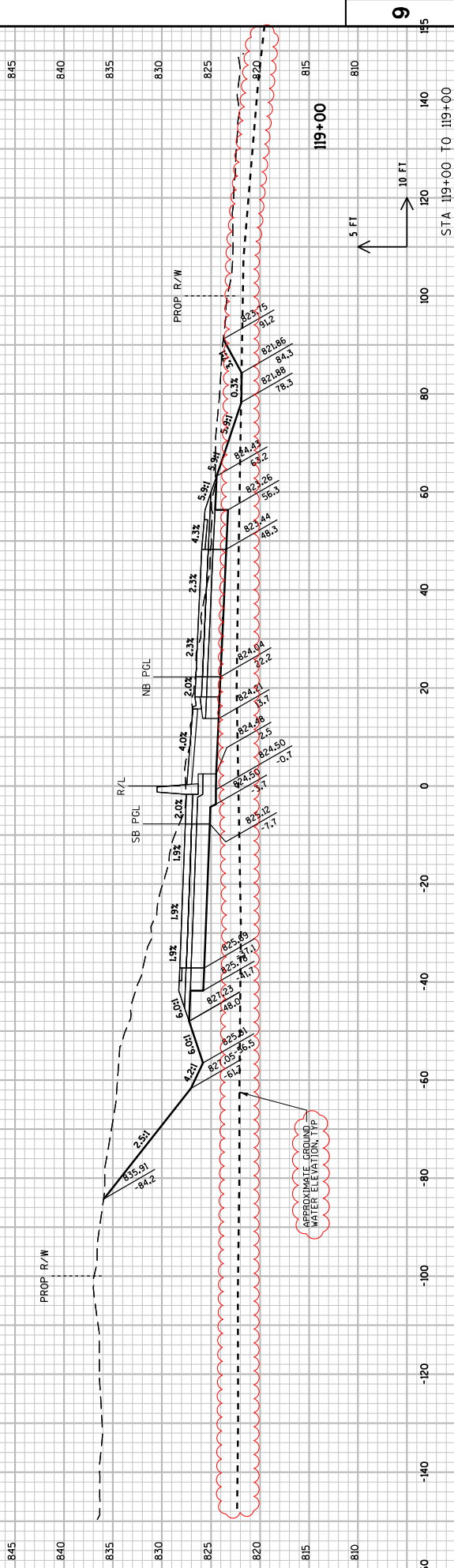
Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1468  
 July 2, 2018

Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1469  
 July 2, 2018



Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1470  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A

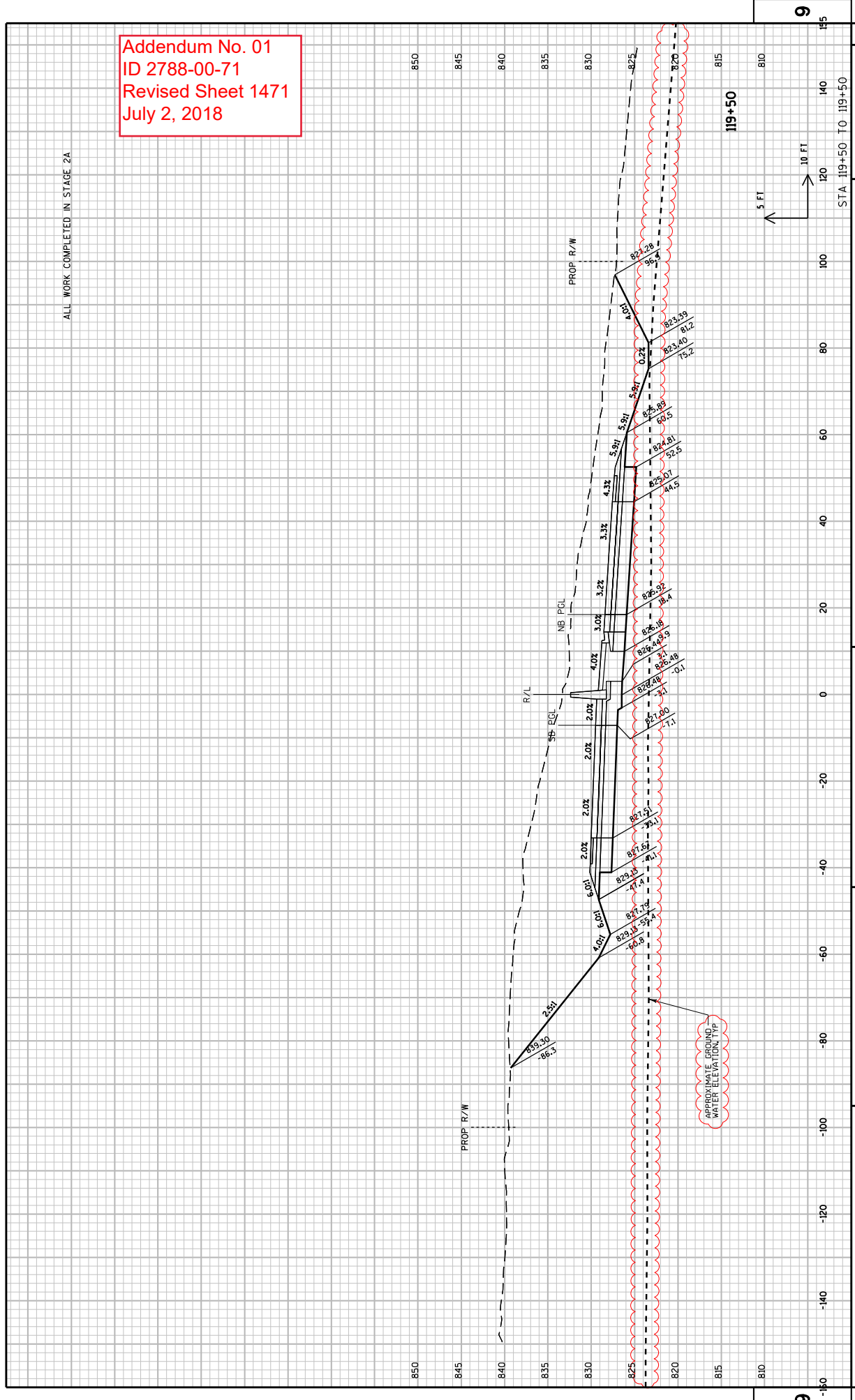


PROJECT NO: 2788-00-71  
 COUNTY: WAUKESHA  
 CROSS SECTIONS: WAUKESHA BYPASS  
 SHEET 1470  
 STA 119+00 TO 119+00  
 SHEET 1470  
 WISDOT/CADD SHEET 21

FILE NAME : \\wplw00\lmgpr\proj\59971\1\cadd\by\_090201\_xs\_wb.dgn  
 PLOT DATE : 6/26/2018 10:24:41 AM  
 PLOT BY : Tm.L.Pb.jm  
 PLOT SCALE : 20:1

Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1471  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A

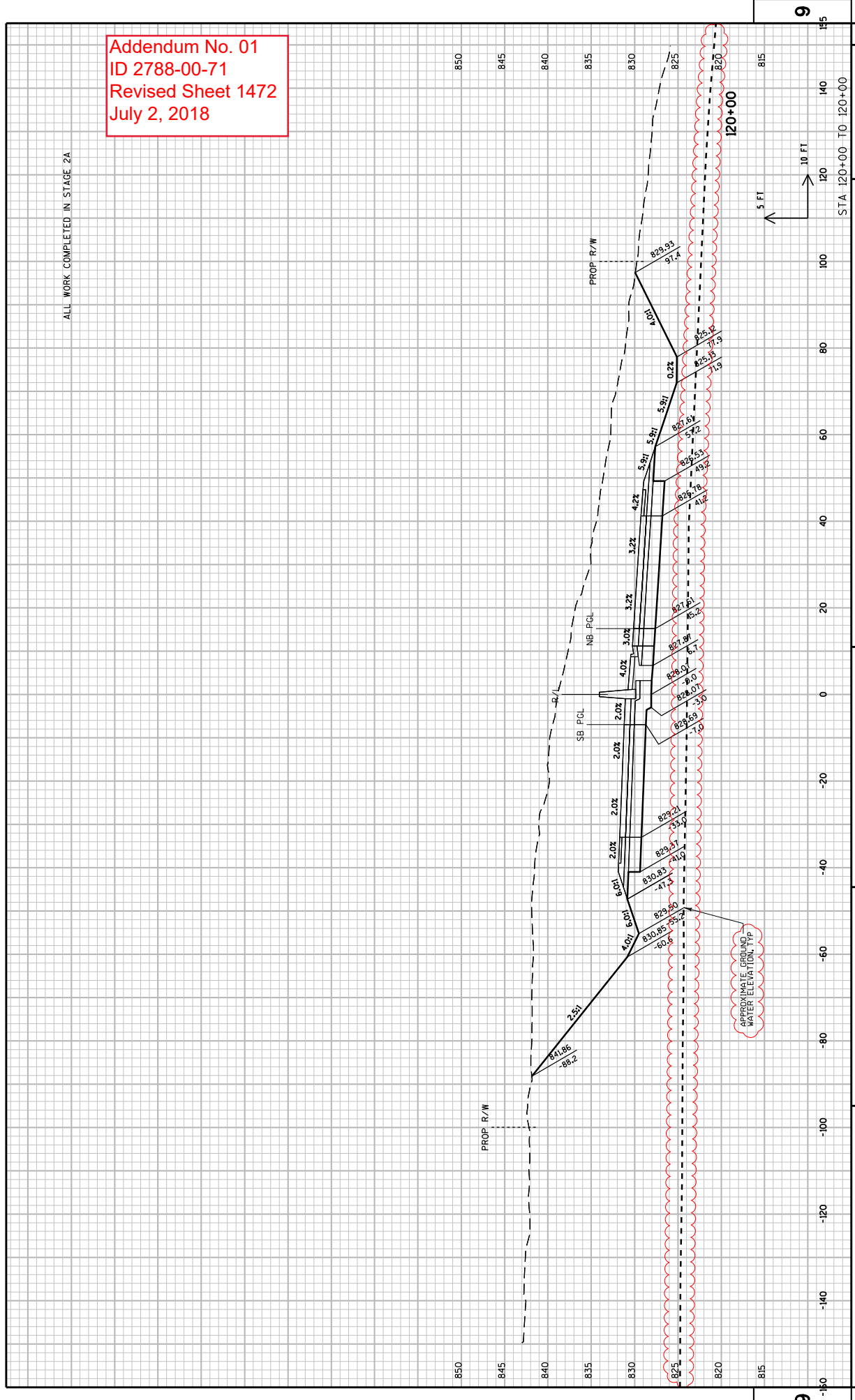


PROJECT NO: 2788-00-71  
 COUNTY: WAUKESHA  
 CROSS SECTIONS: WAUKESHA BYPASS  
 SHEET 1471  
 STA 119+50 TO 119+50

FILE NAME : \\wpl1w00\lmgpr\proj\59971\1\cadd\by\_090201\_xs\_mb.dgn  
 PLOT BY : Tm.L.P@hlm  
 PLOT DATE : 6/26/2018 10:24:42 AM  
 PLOT SCALE : 20:1  
 WISDOT/CADD SHEET 21

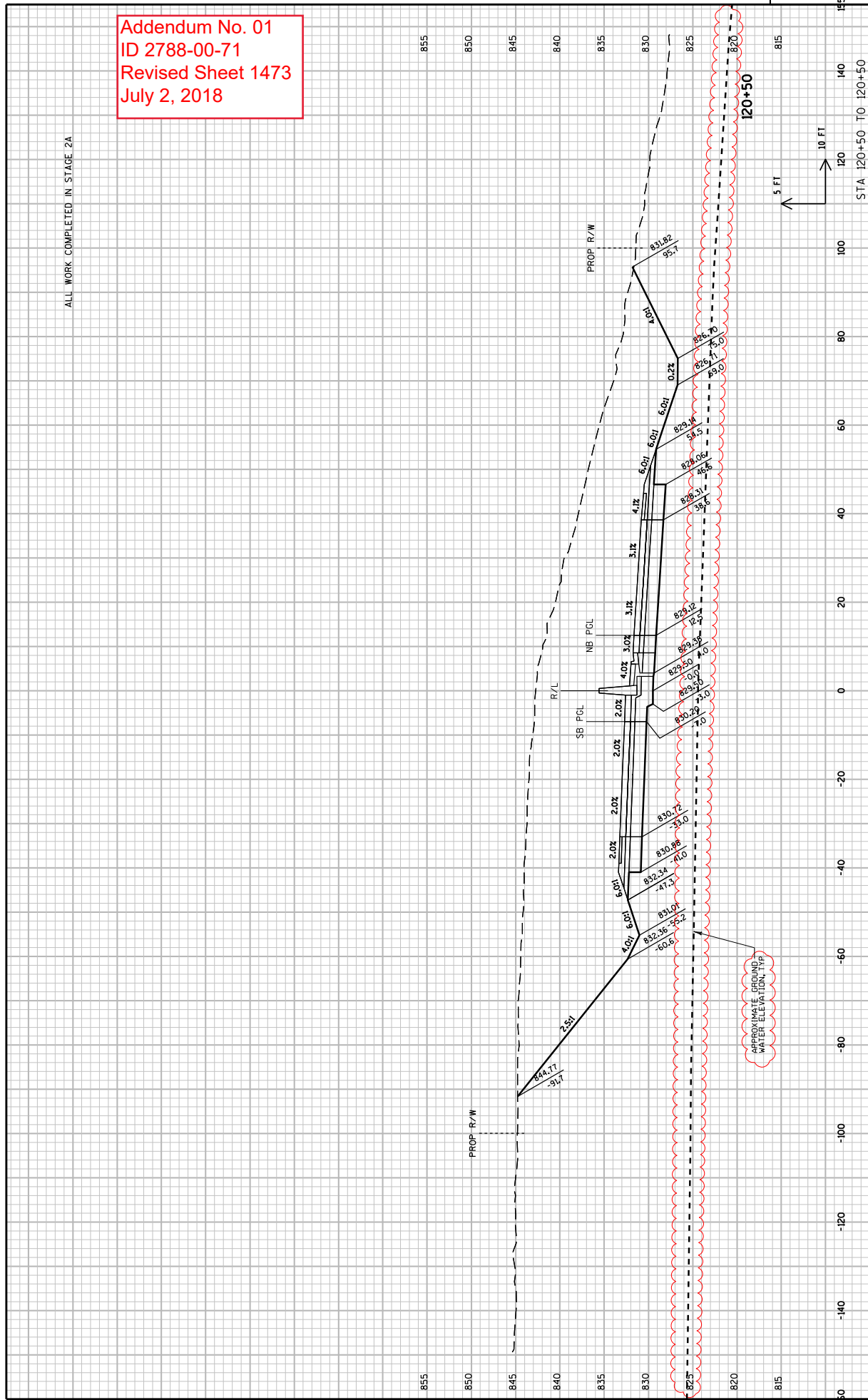
Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1472  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A



Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1473  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A

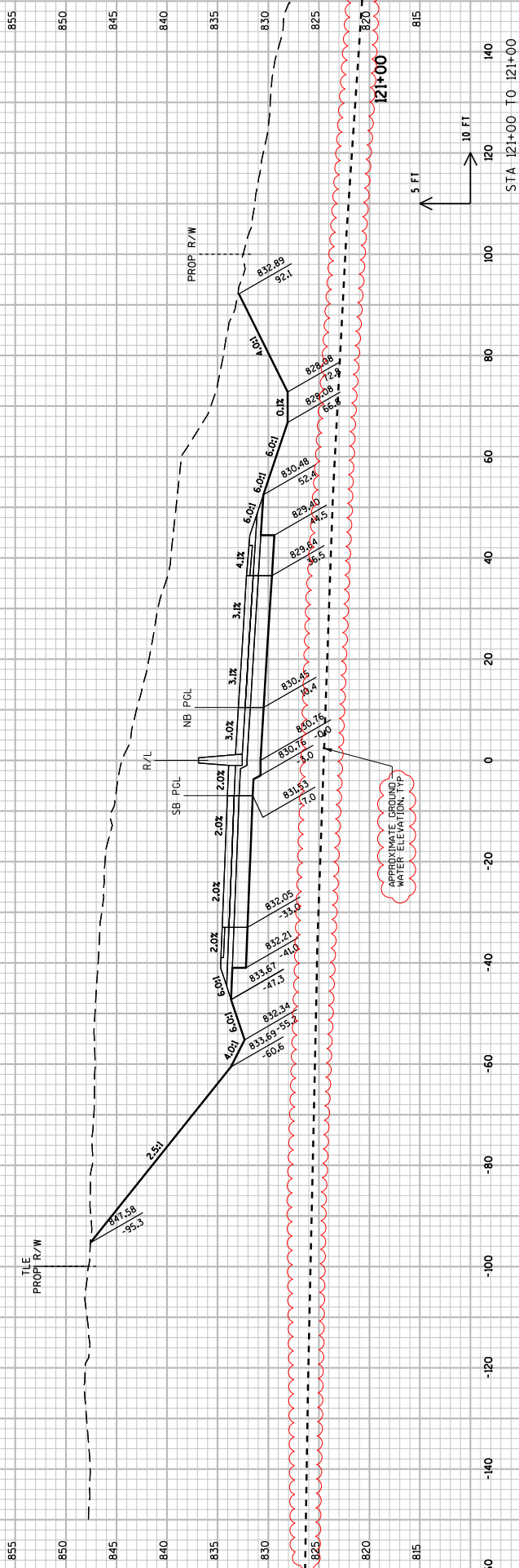


PROJECT NO: 2788-00-71  
 COUNTY: WAUKESHA  
 CROSS SECTIONS: WAUKESHA BYPASS  
 SHEET 1473  
 E

FILE NAME : \\nrl1w00\leng\proj\59971\1\cadd\wy\_090201\_xs\_mb.dgn  
 PLOT DATE : 6/26/2018 10:24:44 AM  
 PLOT BY : tmlp18  
 PLOT SCALE : 20:1  
 WISDOT/CADD SHEET 21

ALL WORK COMPLETED IN STAGE 2A

Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1474  
July 2, 2018



9

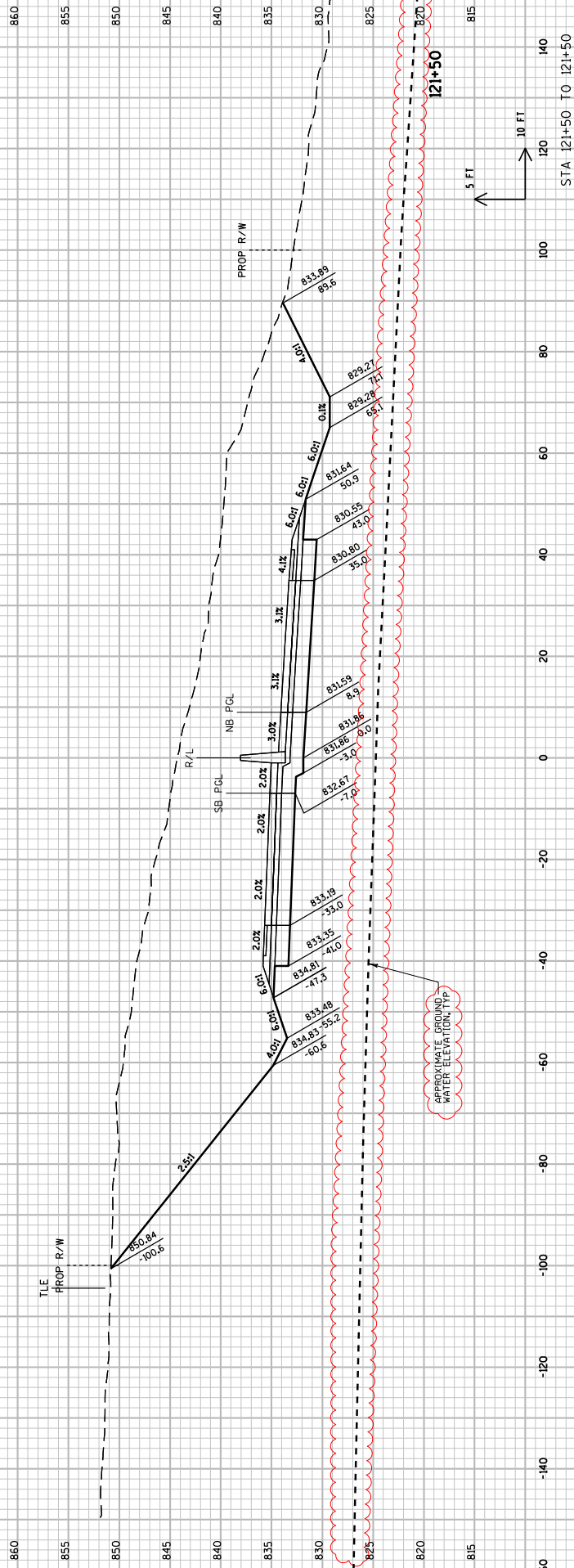
9

PROJECT NO: 2788-00-71      COUNTY: WAUKESHA      CROSS SECTIONS: WAUKESHA BYPASS      SHEET 1474      E

FILE NAME : \\wplw00\lmp\proj\59971\1\cadd\by\_090201\_xs\_wb.dgn      PLOT BY : twj\lpe\lm      PLOT DATE : 6/26/2018 10:24:45 AM      PLOT SCALE : 20:1      WISDOT/CADD SHEET 21

Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1475  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A



5 FT  
 10 FT

STA 121+50 TO 121+50

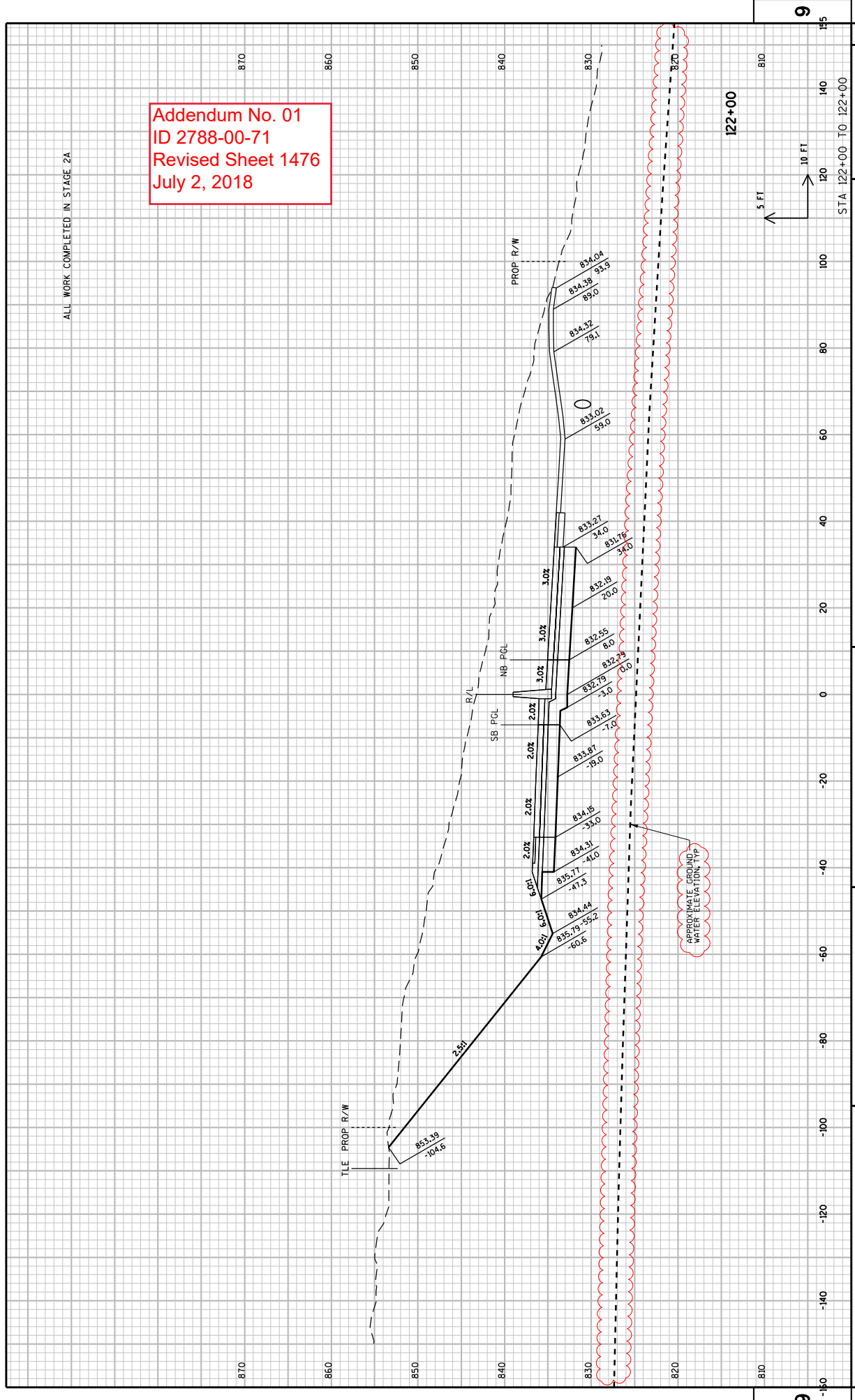
PROJECT NO: 2788-00-71  
 COUNTY: WAUKESHA  
 HWY: WAUKESHA BYPASS  
 CROSS SECTIONS: WAUKESHA BYPASS  
 SHEET 1475  
 E

FILE NAME : \\wplw00\lmgpr\proj\59971\1\cadd\by\_090201\_xs\_mb.dgn  
 PLOT BY : fhw.lpw@dnr.wisconsin.gov  
 PLOT DATE : 6/26/2018 10:24:46 AM  
 PLOT SCALE : 20:1  
 WISDOT/CADD SHEET 21



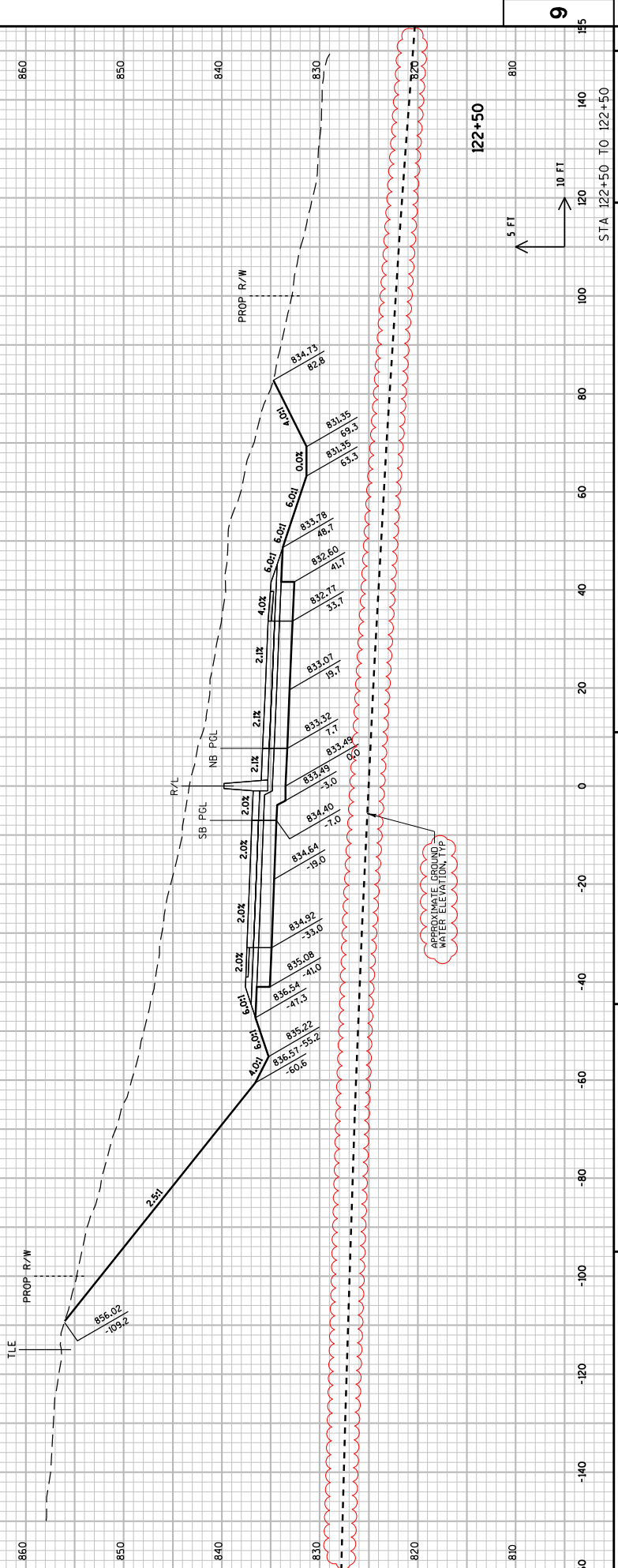
ALL WORK COMPLETED IN STAGE 2A

Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1476  
July 2, 2018



ALL WORK COMPLETED IN STAGE 2A

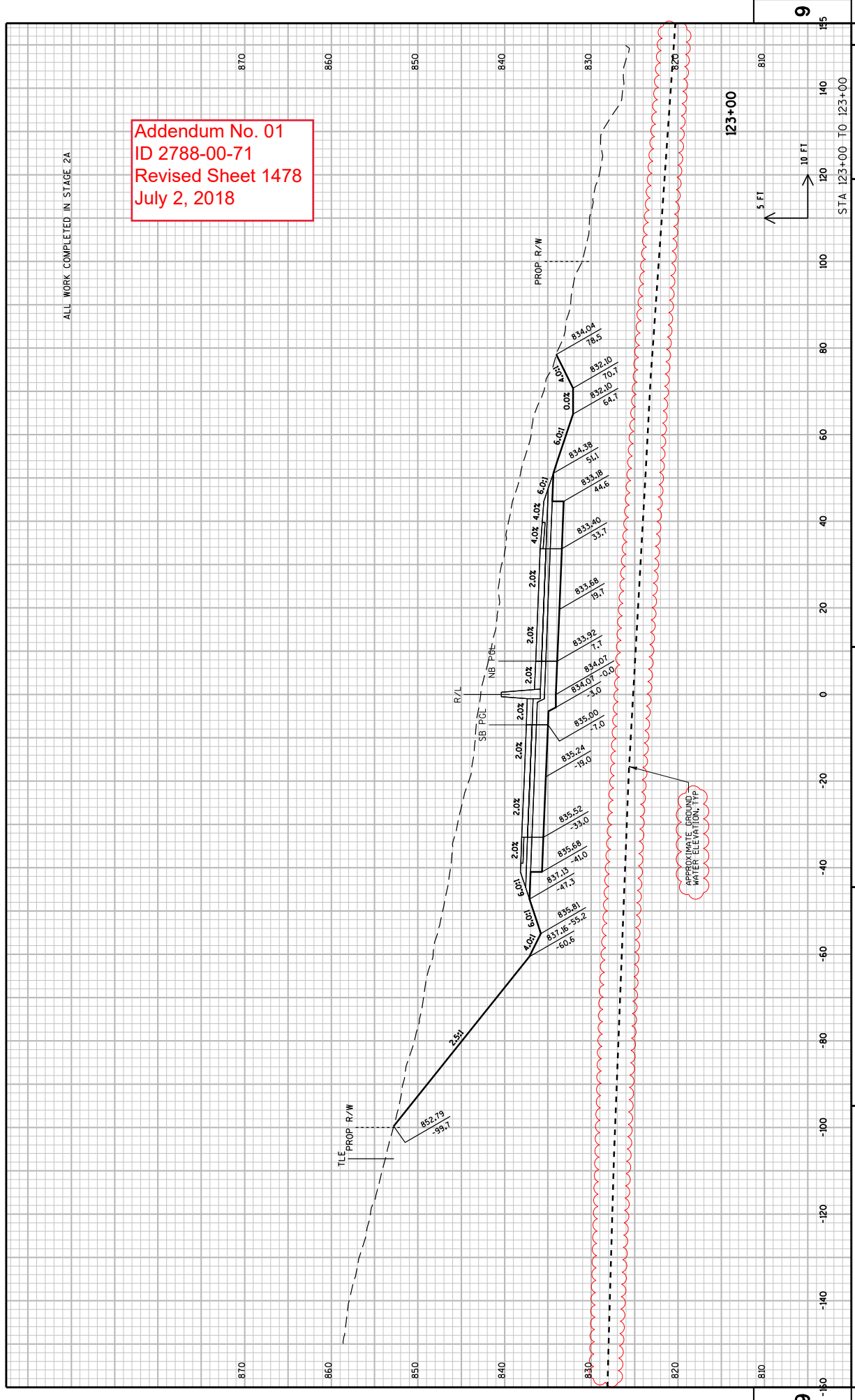
Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1477  
July 2, 2018



PROJECT NO: 2788-00-71  
 COUNTY: WAUKESHA  
 HWY: WAUKESHA BYPASS  
 CROSS SECTIONS: WAUKESHA BYPASS  
 SHEET 1477  
 STA 122+50 TO 122+50  
 PLOT DATE : 6/26/2018 10:24:47 AM  
 PLOT BY : TFW.LP@E.IR  
 PLOT SCALE : 20:1  
 WISDOT/CADD SHEET 21

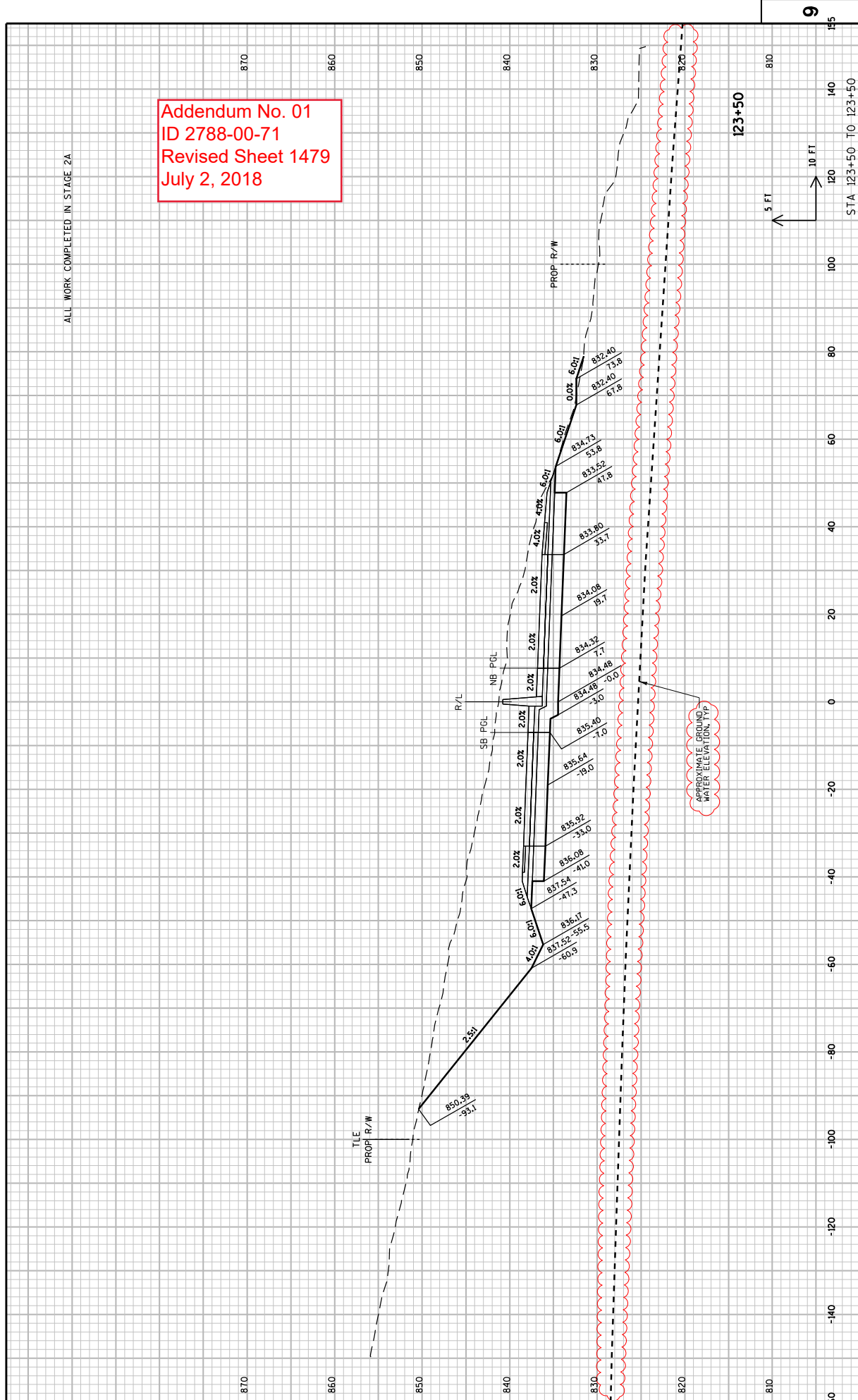
ALL WORK COMPLETED IN STAGE 2A

Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1478  
July 2, 2018



ALL WORK COMPLETED IN STAGE 2A

Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1479  
July 2, 2018



PROJECT NO: 2788-00-71  
 COUNTY: WAUKESHA  
 HWY: WAUKESHA BYPASS  
 CROSS SECTIONS: WAUKESHA BYPASS  
 SHEET 1479  
 STA 123+50 TO 123+50  
 PLOT DATE : 6/26/2018 10:24:49 AM  
 PLOT BY : TFW.LP@i.in  
 PLOT SCALE : 20:1  
 WISDOT/CADD SHEET 21

Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1480  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A

860

850

840

830

820

810

9

PROP R/W

R/L

SB PCL

2.0%

848.19

-96.8

2.53%

6.0%

836.42

-55.3

6.0%

837.71

-50.7

6.0%

831.71

-41.2

2.0%

836.31

-41.0

2.0%

836.16

-33.0

2.0%

835.87

-19.0

2.0%

834.70

-3.0

2.0%

834.70

0.0

2.0%

834.55

7.1

2.0%

834.31

19.7

2.0%

834.03

33.7

4.0%

833.69

50.3

4.0%

834.91

56.1

6.0%

832.76

63.1

6.0%

832.76

63.1

ON T.M.

PROP R/W

APPROXIMATE GROUND  
 WATER ELEVATION TYP

124+00

5 FT

10 FT

STA 124+00 TO 124+00

SHEET 1480

CROSS SECTIONS: WAUKESHA BYPASS

COUNTY: WAUKESHA

HWY: WAUKESHA BYPASS

PROJECT NO: 2788-00-71

PLOT SCALE: 20:1

PLOT BY: TFW.LP@FIRM

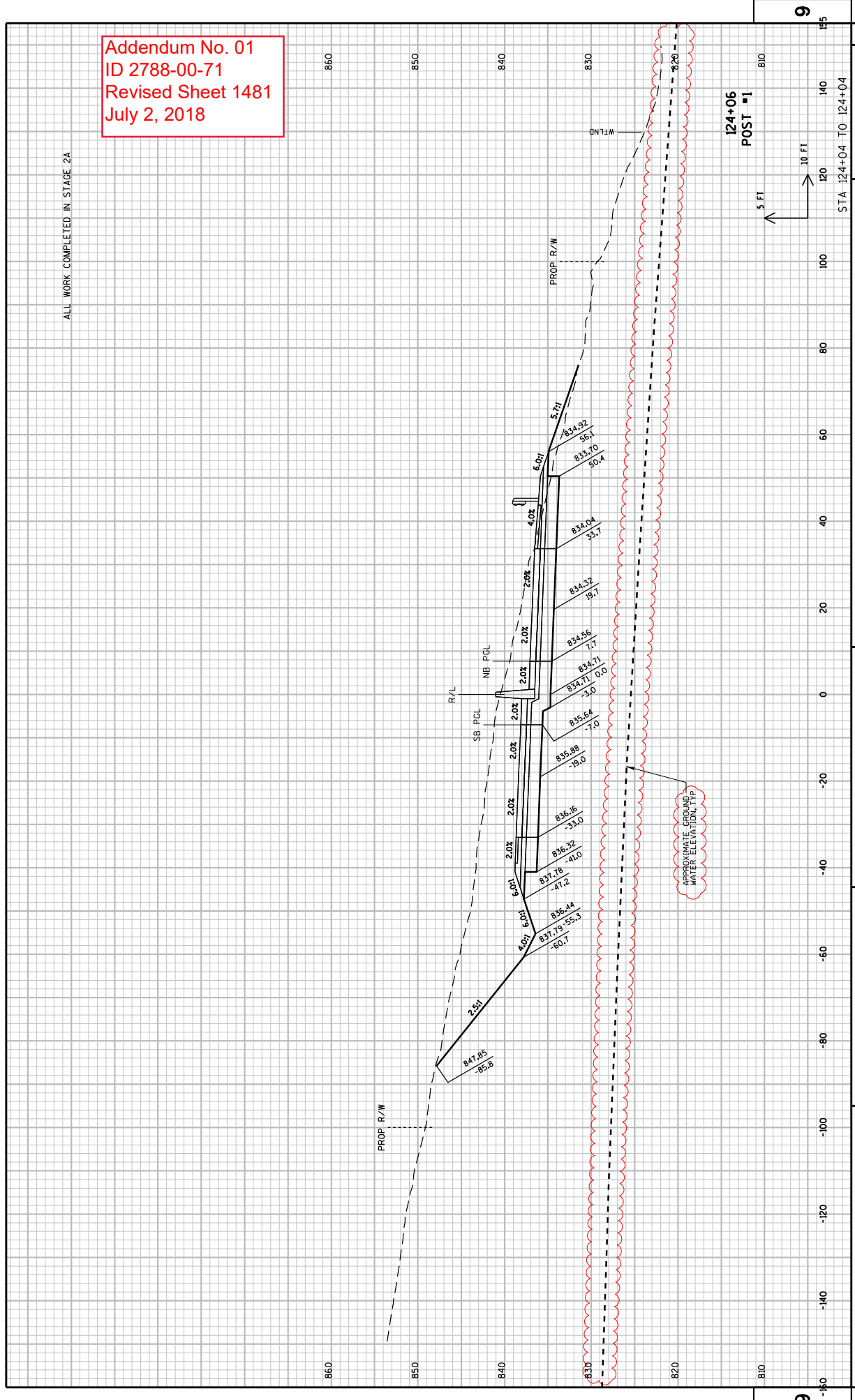
PLOT DATE: 6/26/2018 10:24:50 AM

FILE NAME: \\nw11w00\jng\proj\59971\1\cadd\by\_090201\_xs\_wb.dgn

WISDOT/CADD SHEET 21

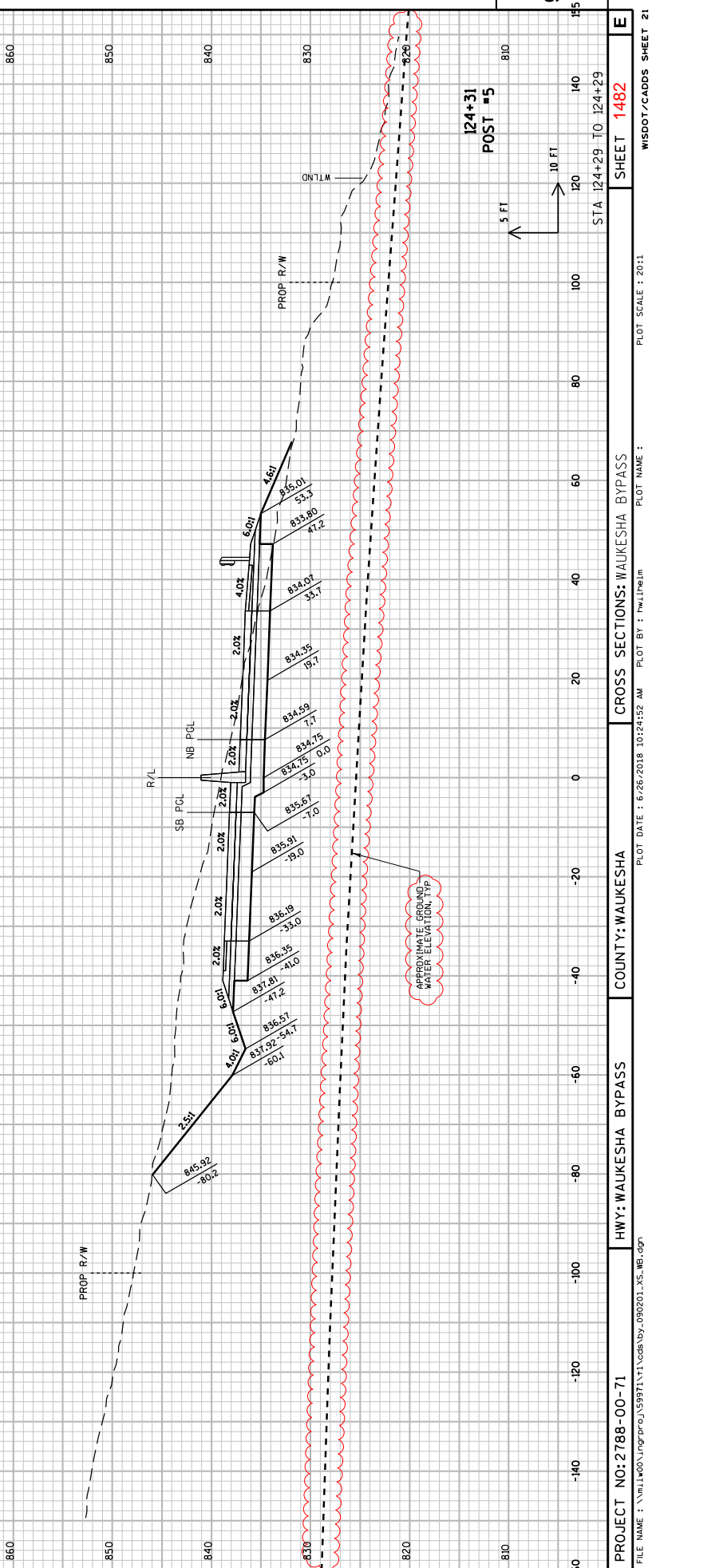
Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1481  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A



Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1482  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A



9

9

PROJECT NO: 2788-00-71

HWY: WAUKESHA BYPASS

COUNTY: WAUKESHA

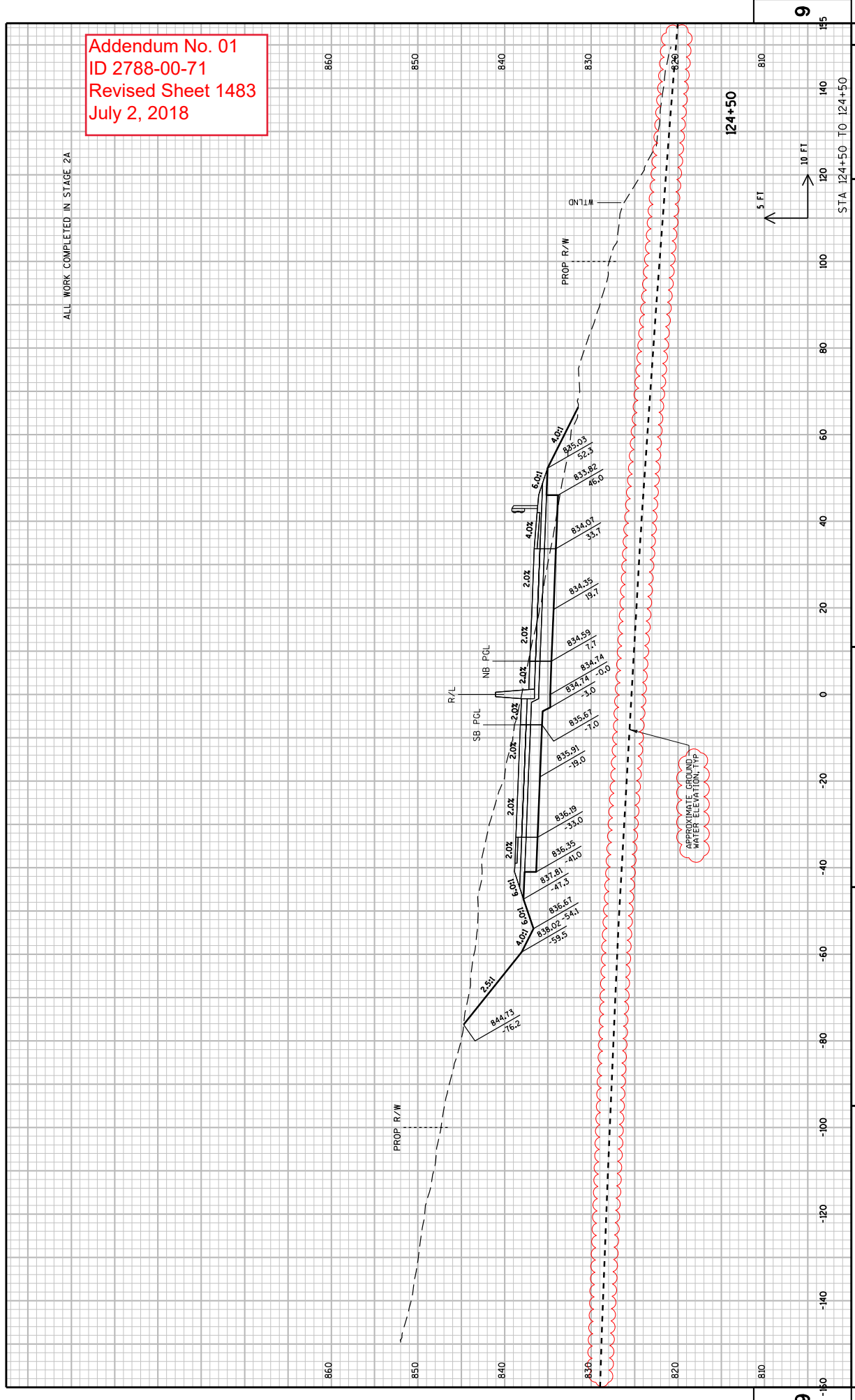
CROSS SECTIONS: WAUKESHA BYPASS

SHEET 1482

E

Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1483  
 July 2, 2018

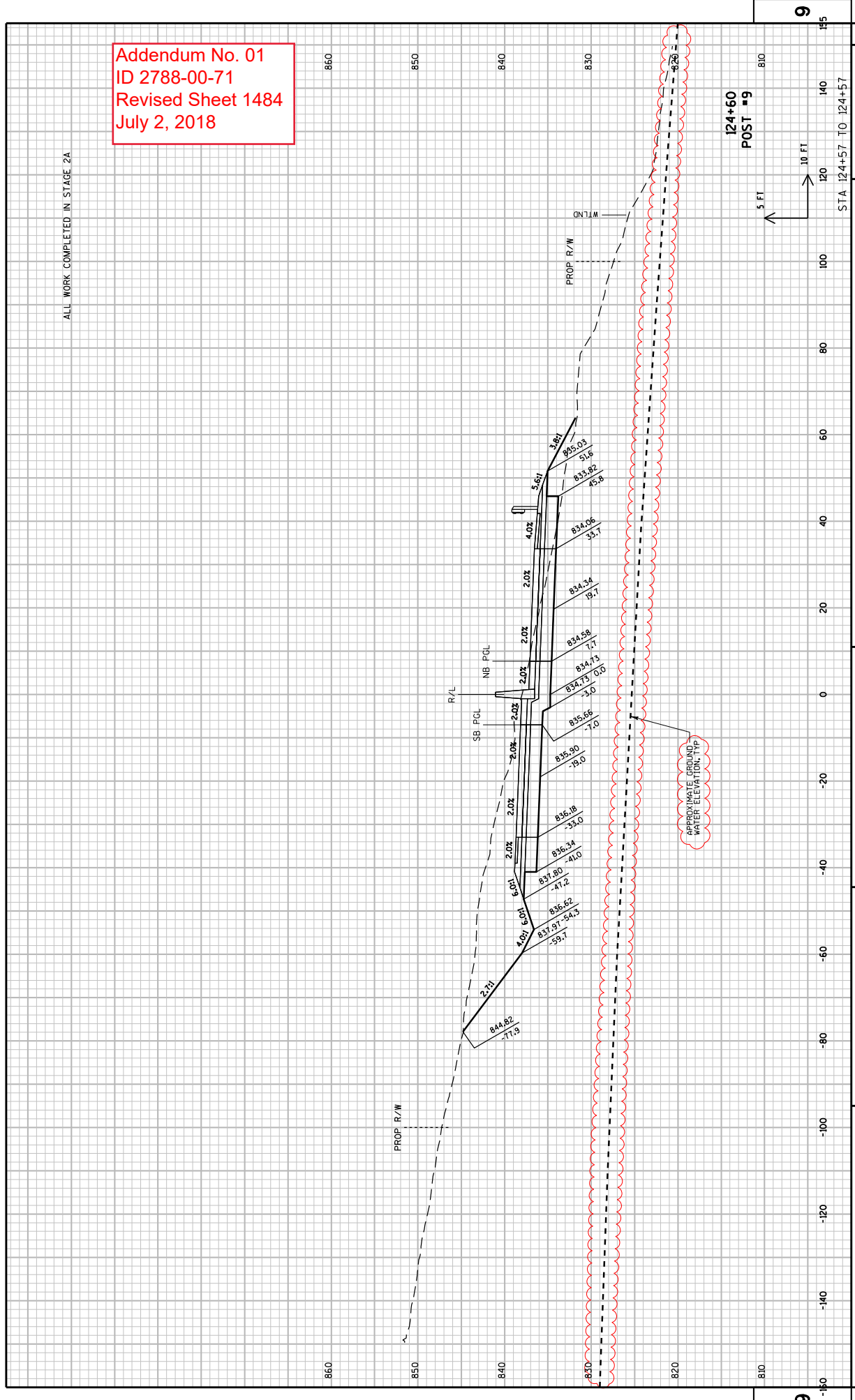
ALL WORK COMPLETED IN STAGE 2A





Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1484  
 July 2, 2018

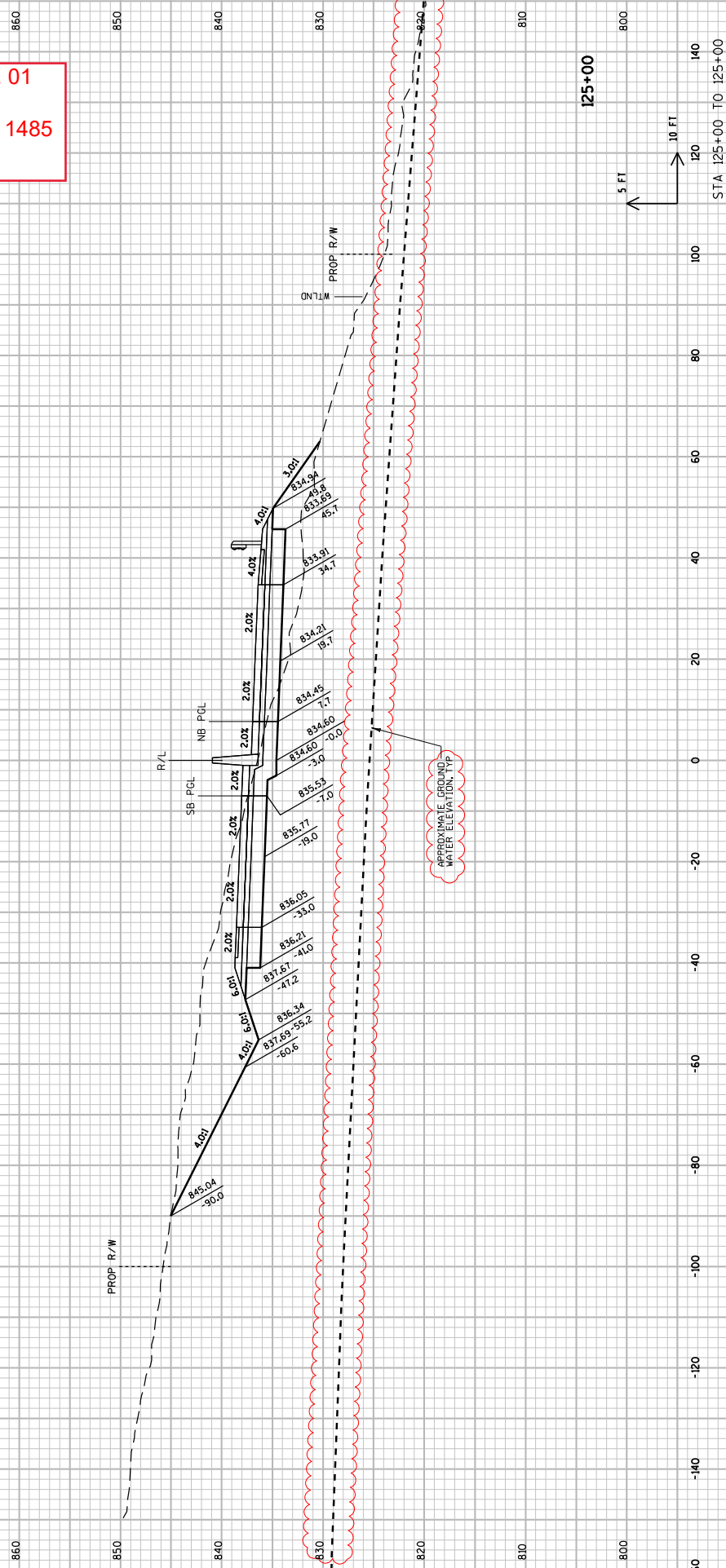
ALL WORK COMPLETED IN STAGE 2A



APPROXIMATE GROUND WATER ELEVATION (IP)

ALL WORK COMPLETED IN STAGE 2A

Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1485  
July 2, 2018

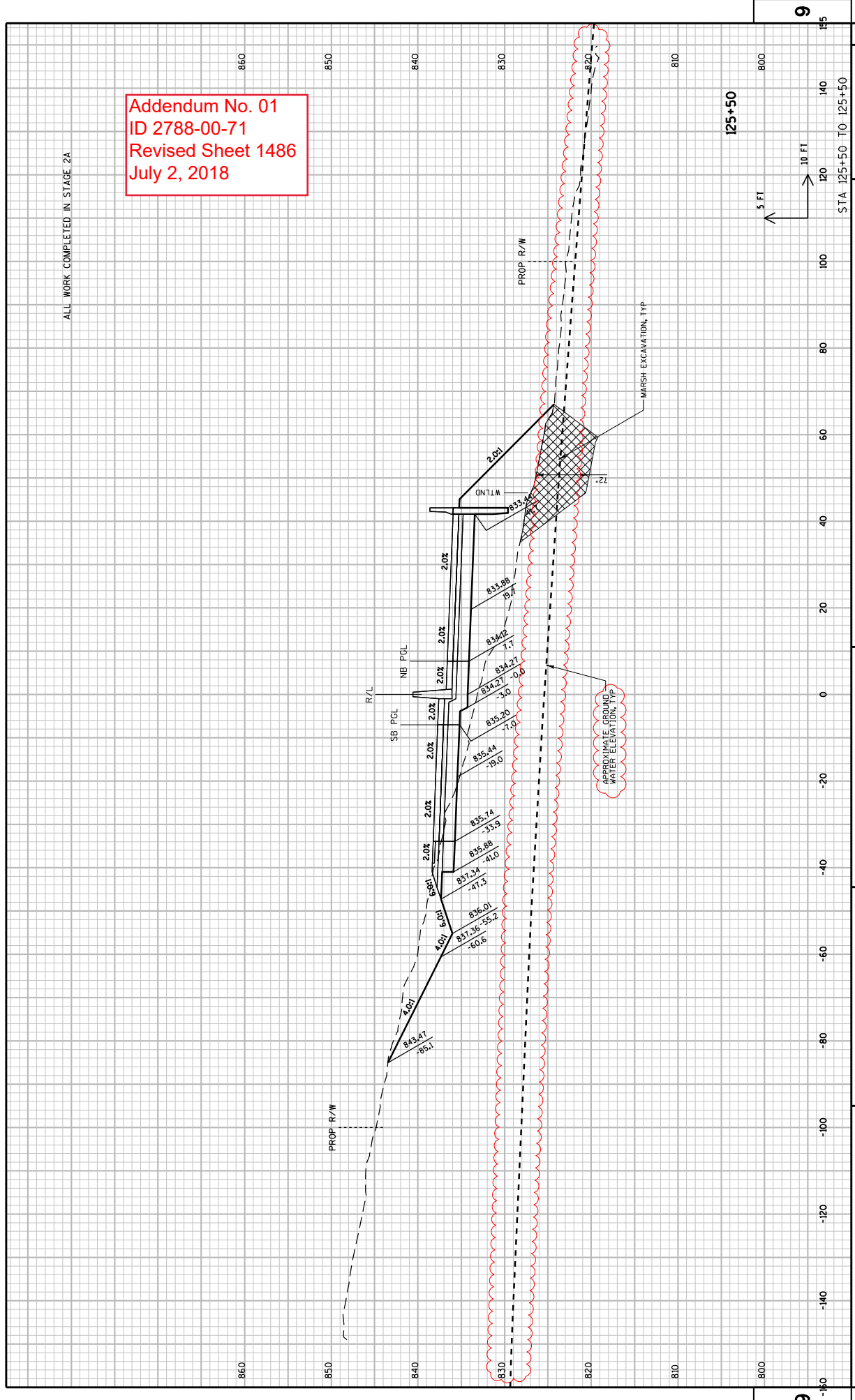


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9

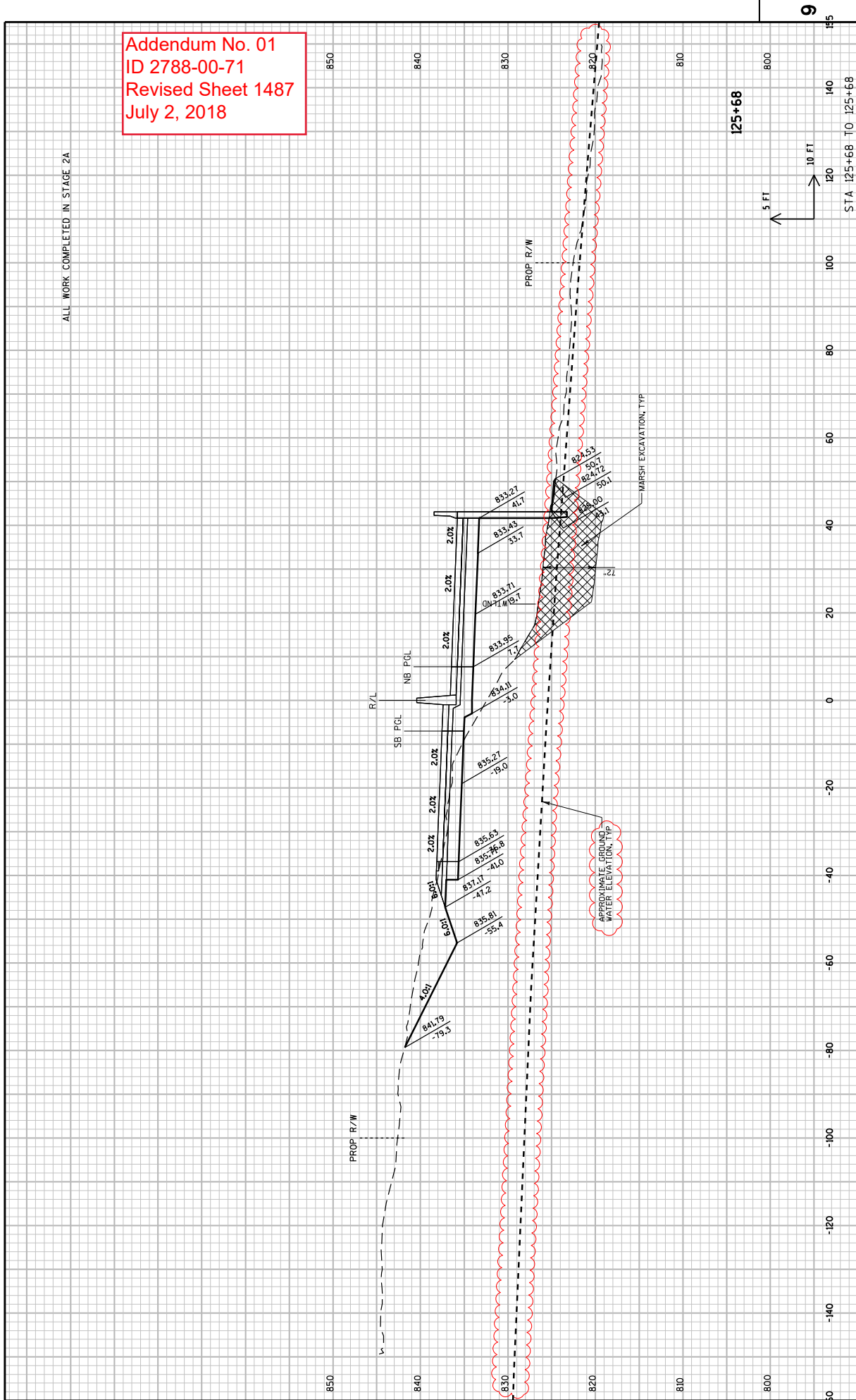
ALL WORK COMPLETED IN STAGE 2A

Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1486  
July 2, 2018



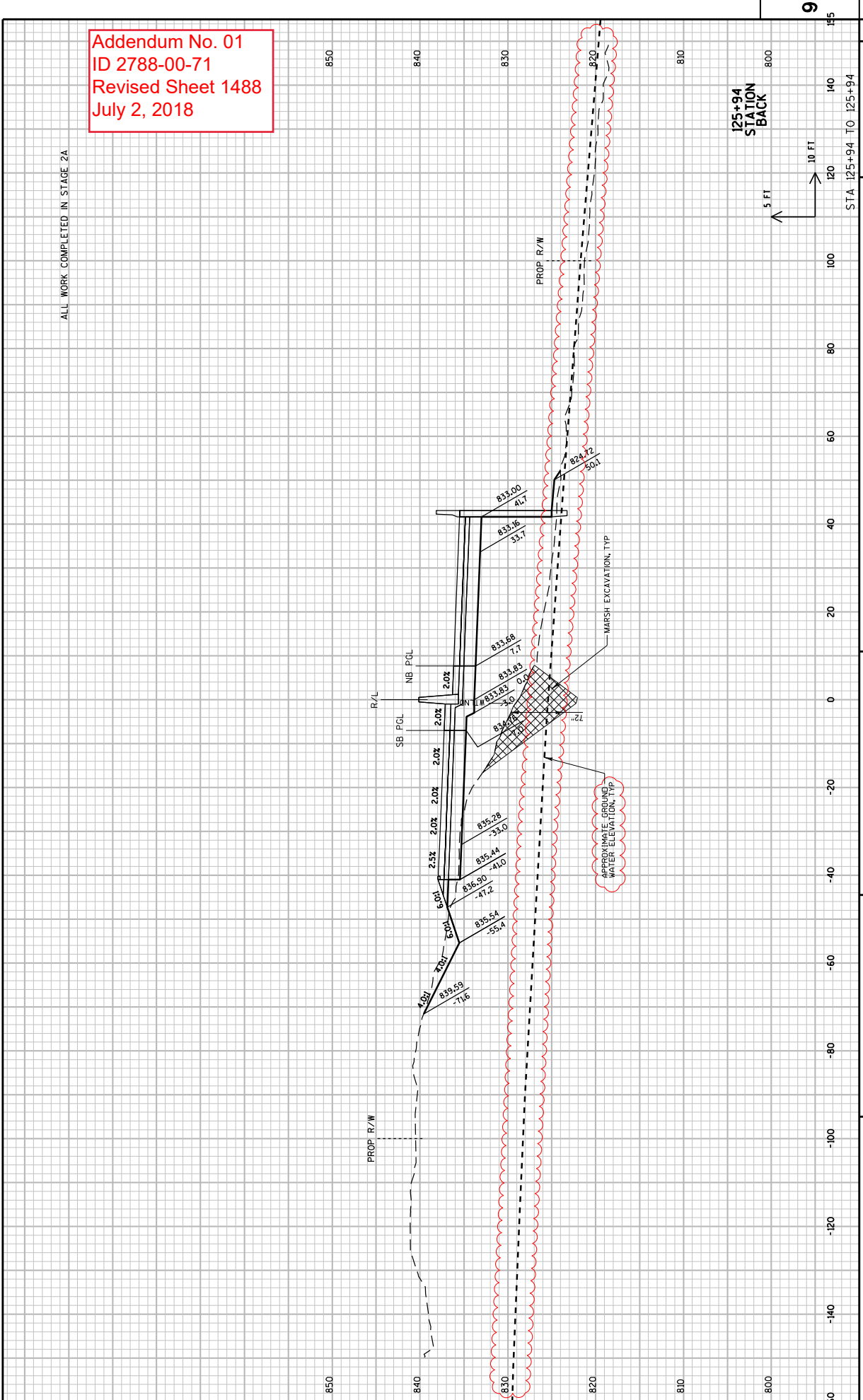
Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1487  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A



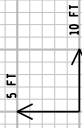
Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1488  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A



9

125+94  
 STATION  
 BACK



STA 125+94 TO 125+94

SHEET 1488

WISDOT/CADD SHEET 21

CROSS SECTIONS: WAUKESHA BYPASS

COUNTY: WAUKESHA

HWY: WAUKESHA BYPASS

PROJECT NO: 2788-00-71

PLOT SCALE: 20:1

PLOT NAME:

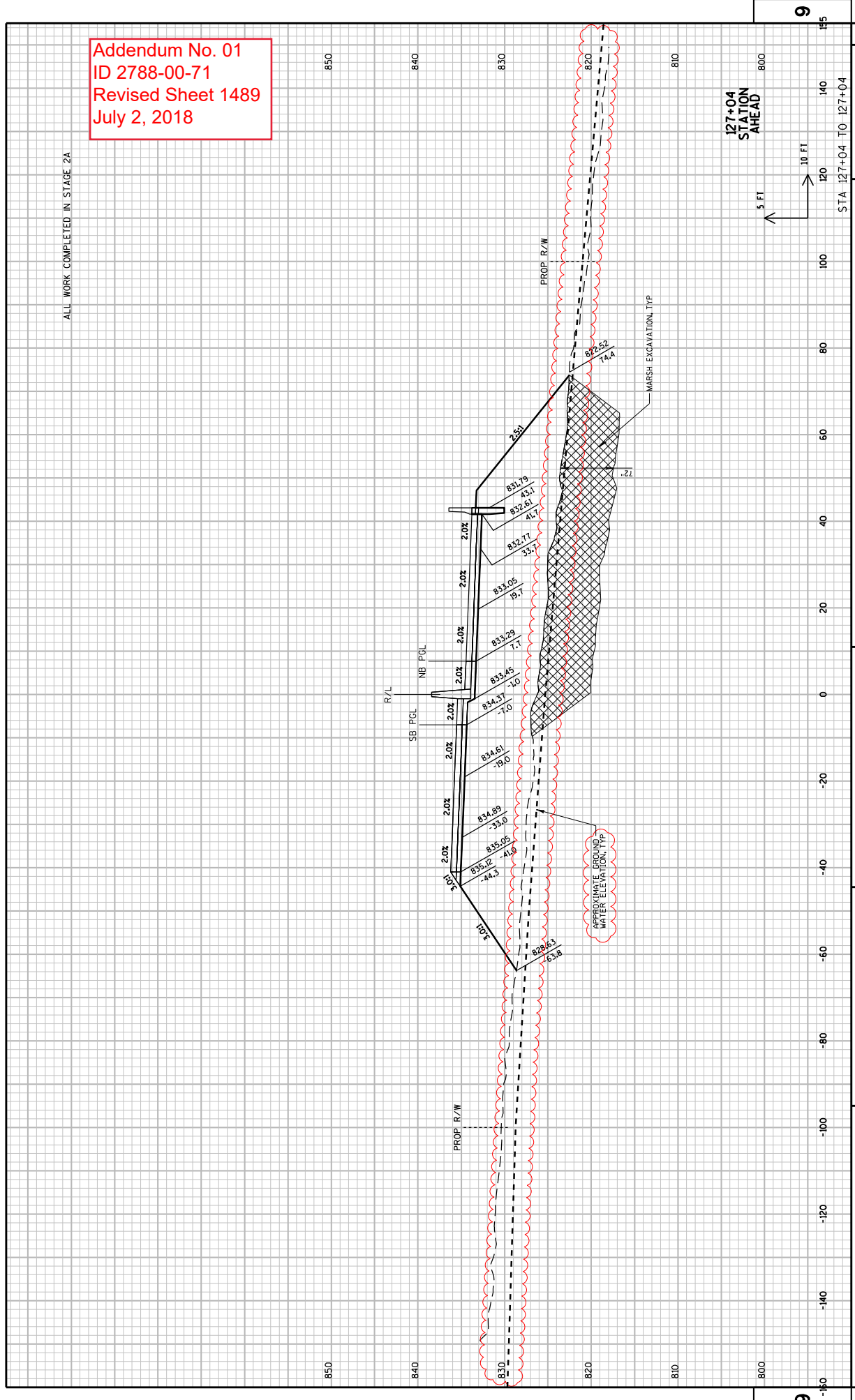
PLOT BY: TFW.LP@WI

PLOT DATE: 6/26/2018 10:25:01 AM

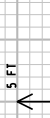
FILE NAME: \\wblw001\proj\proj\59971\1\cadd\wy\_090201\_xs\_wb.dgn

Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1489  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A

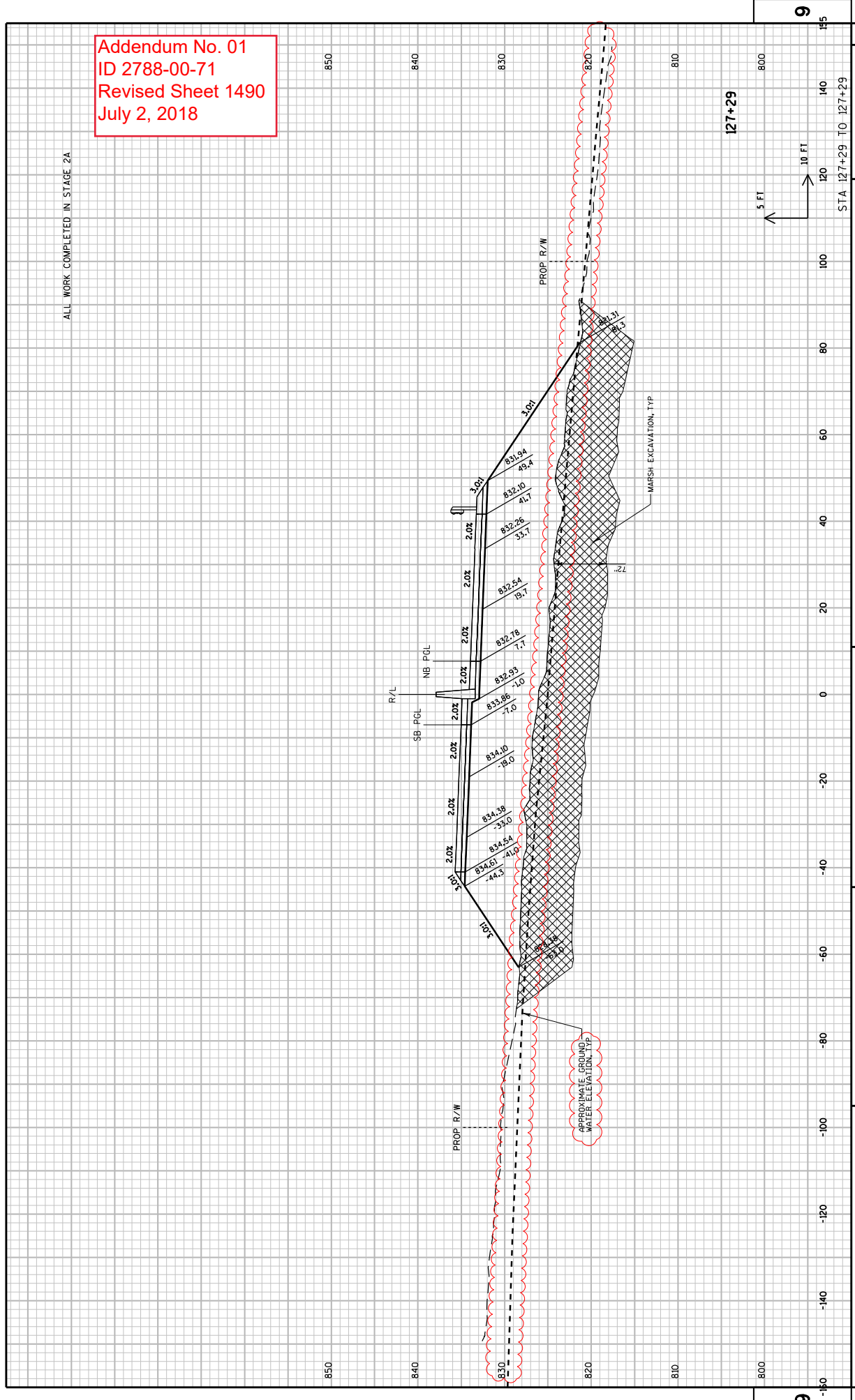


127+04  
 STATION  
 AHEAD

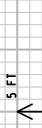


Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1490  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A



127+29



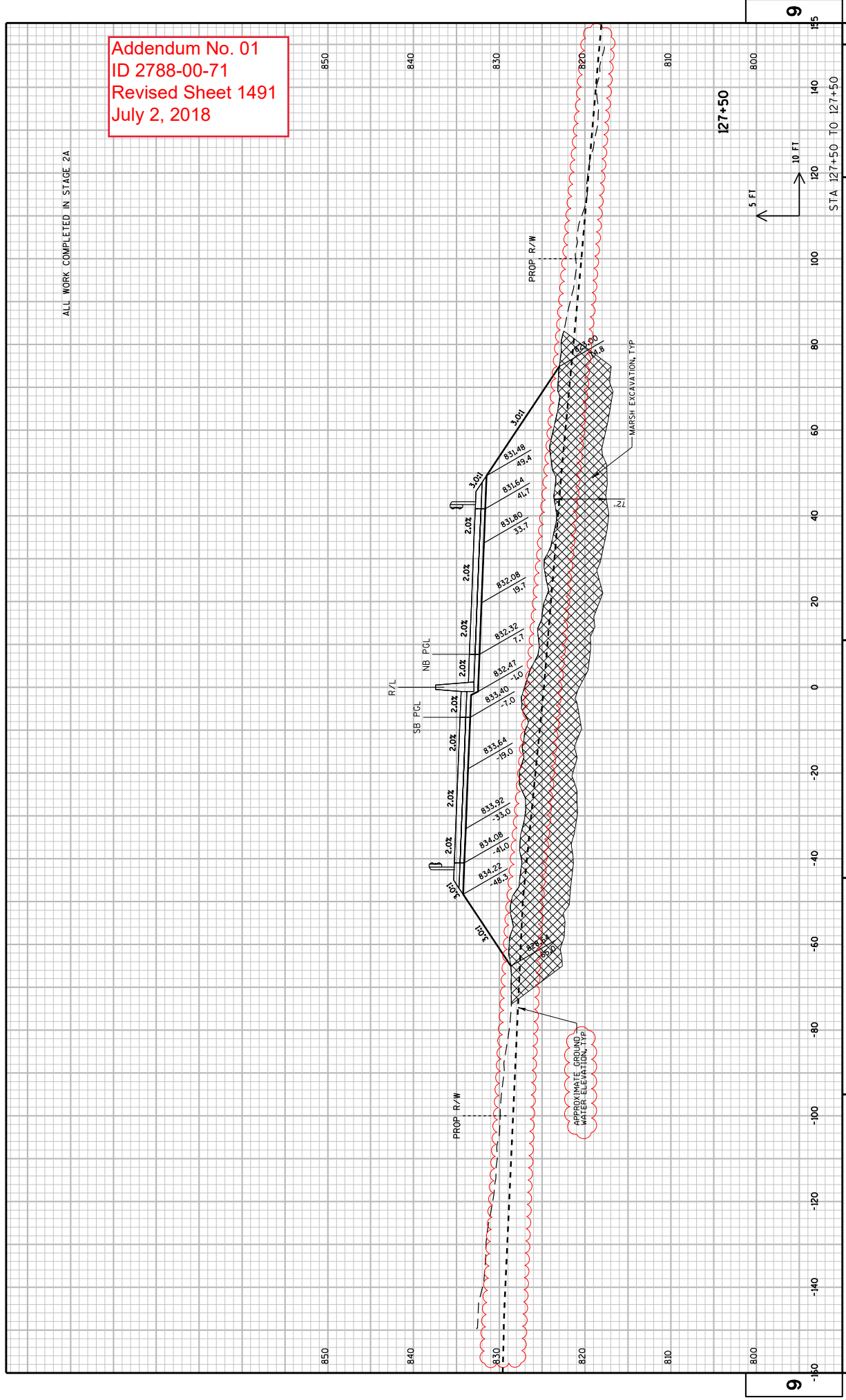
850  
840  
830  
820  
810  
800

9  
150  
140  
130  
120  
110  
100  
90  
80  
70  
60  
50  
40  
30  
20  
10  
0  
-10  
-20  
-30  
-40  
-50  
-60  
-70  
-80  
-90  
-100  
-110  
-120  
-130  
-140  
-150

PROJECT NO: 2788-00-71  
 COUNTY: WAUKESHA  
 HWY: WAUKESHA BYPASS  
 CROSS SECTIONS: WAUKESHA BYPASS  
 SHEET 1490  
 STA 127+29 TO 127+29

Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1491  
 July 2, 2018

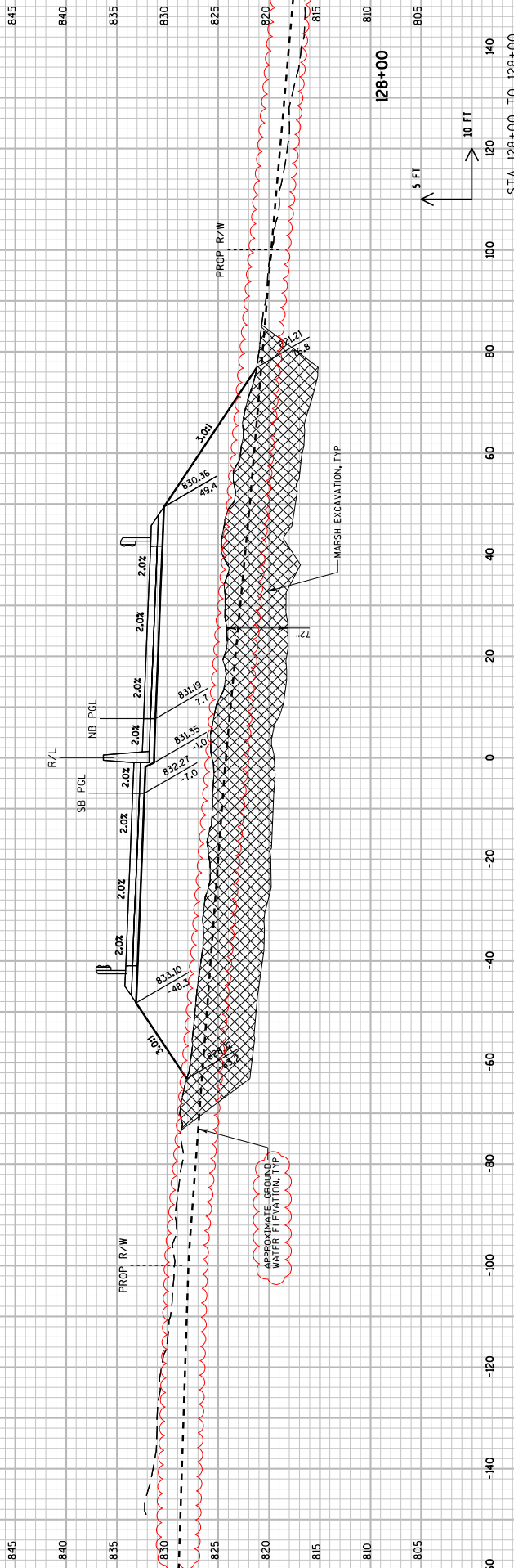
ALL WORK COMPLETED IN STAGE 2A





ALL WORK COMPLETED IN STAGE 2A

Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1492  
July 2, 2018



9

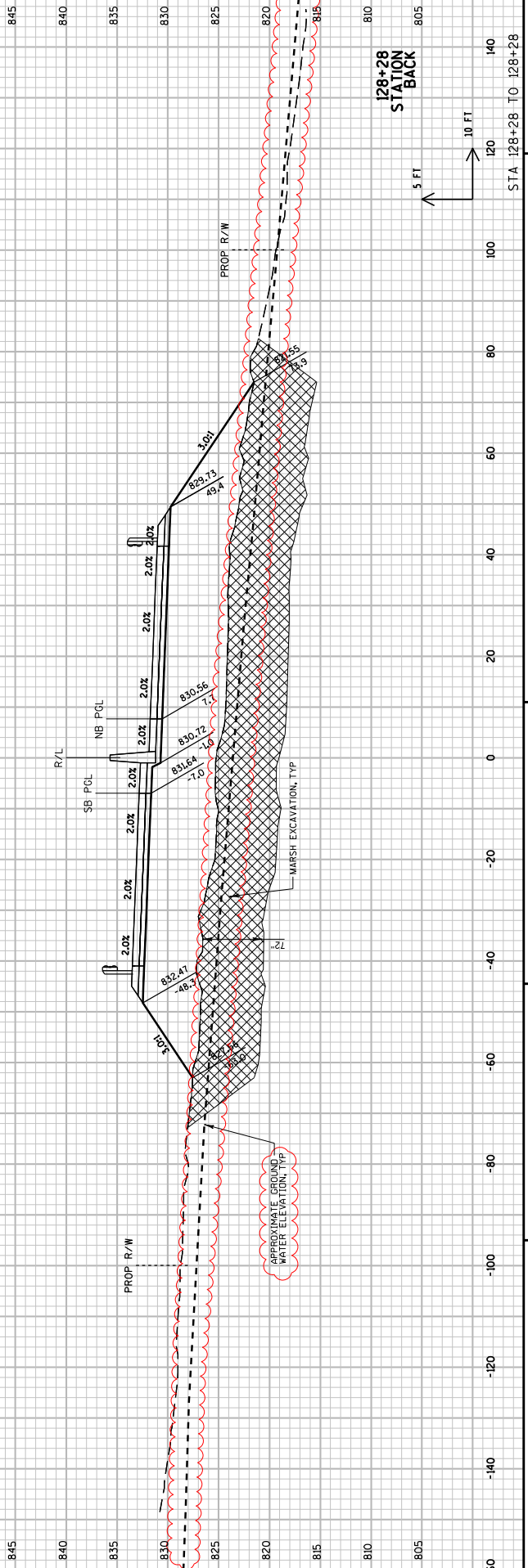
9

PROJECT NO: 2788-00-71  
 COUNTY: WAUKESHA  
 HWY: WAUKESHA BYPASS  
 CROSS SECTIONS: WAUKESHA BYPASS  
 SHEET 1492  
 STA 128+00 TO 128+00  
 PLOT SCALE: 20:1  
 PLOT BY: TFW.LP@ILM  
 WISDOT/CADD SHEET 21

FILE NAME : \\wplw00\lmgpr\proj\59971\1\cadd\by\_090201\_xs\_wb.dgn  
 PLOT DATE : 6/26/2018 10:26:53 AM

Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1493  
July 2, 2018

ALL WORK COMPLETED IN STAGE 2A



9

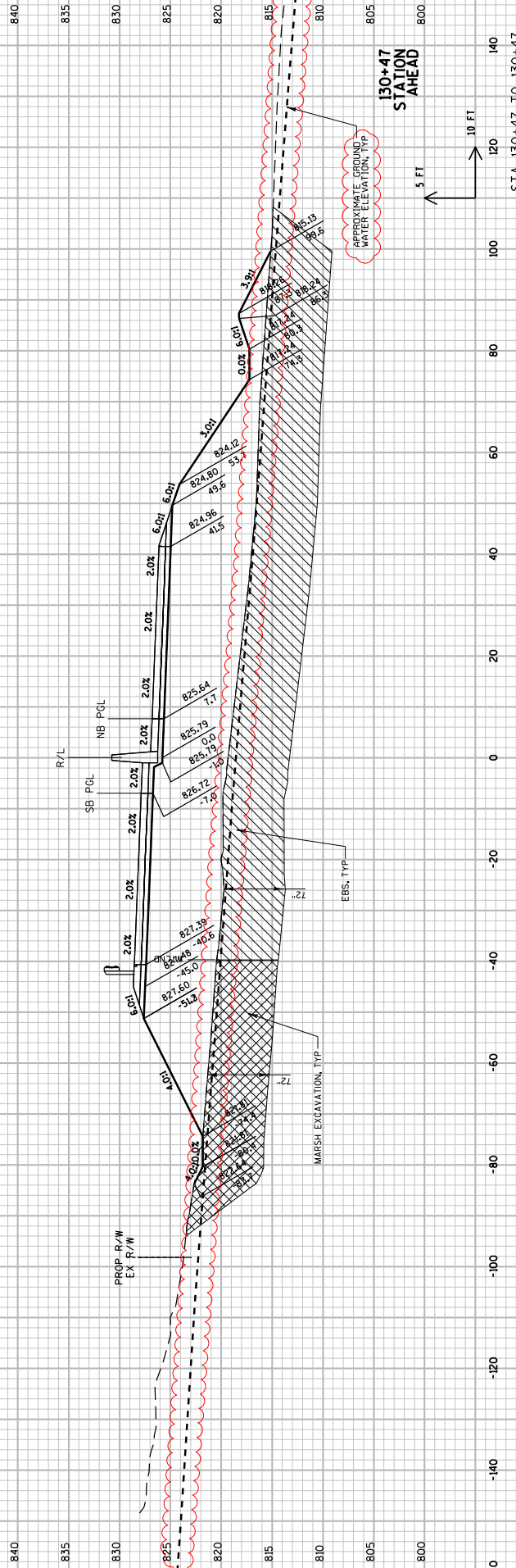
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PROJECT NO: 2788-00-71  
COUNTY: WAUKESHA  
HWY: WAUKESHA BYPASS  
CROSS SECTIONS: WAUKESHA BYPASS  
SHEET 1493  
STA 128+28 TO 128+28  
WISDOT/CADD SHEET 21

FILE NAME : \\wpl1w00\lmp\proj\59971\1\cadd\by\_090201\_xs\_wb.dgn  
PLOT DATE : 6/26/2018 10:25:05 AM  
PLOT BY : TWLPLM  
PLOT SCALE : 20:1

Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1494  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A



9

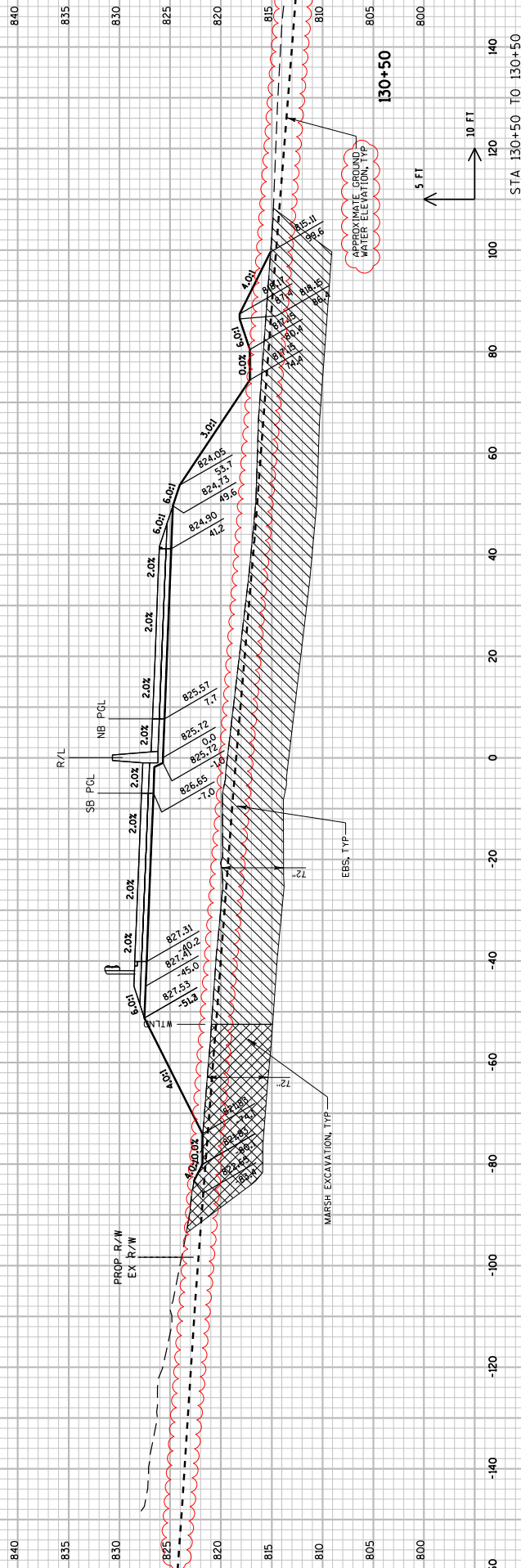
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PROJECT NO: 2788-00-71 COUNTY: WAUKESHA CROSS SECTIONS: WAUKESHA BYPASS SHEET 1494

FILE NAME : \\wplw001\proj\proj\59971\1\cadd\by\_090201\_xs\_wb.dgn PLOT DATE : 6/26/2018 10:25:06 AM PLOT BY : TWL\JPH PLOT SCALE : 20:1 WISDOT/CADD SHEET 21

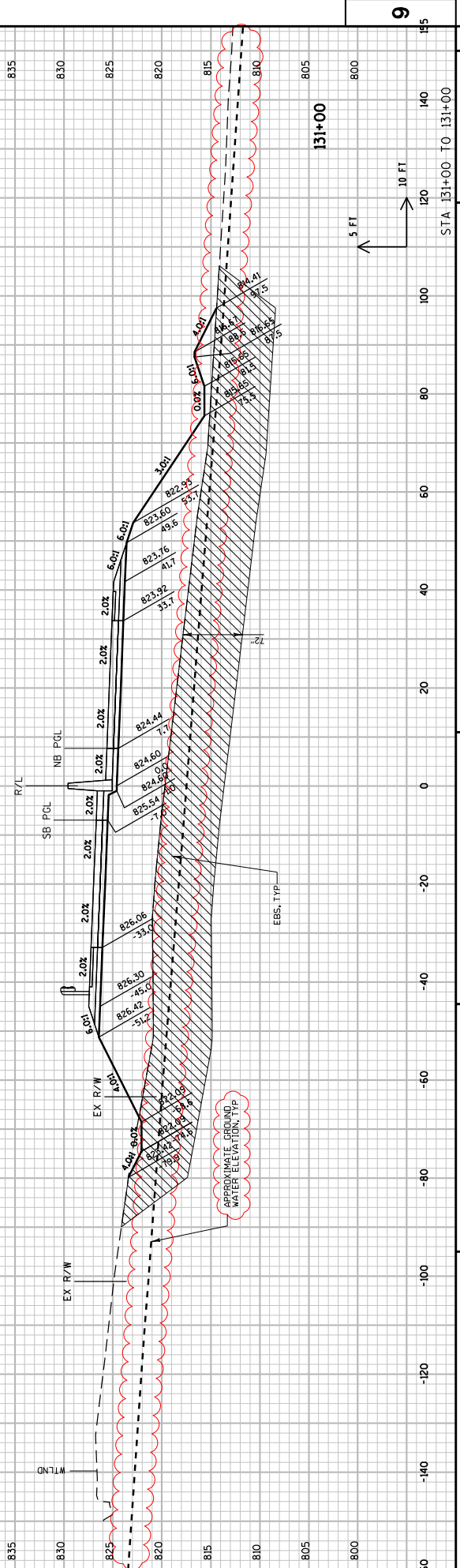
Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1495  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A



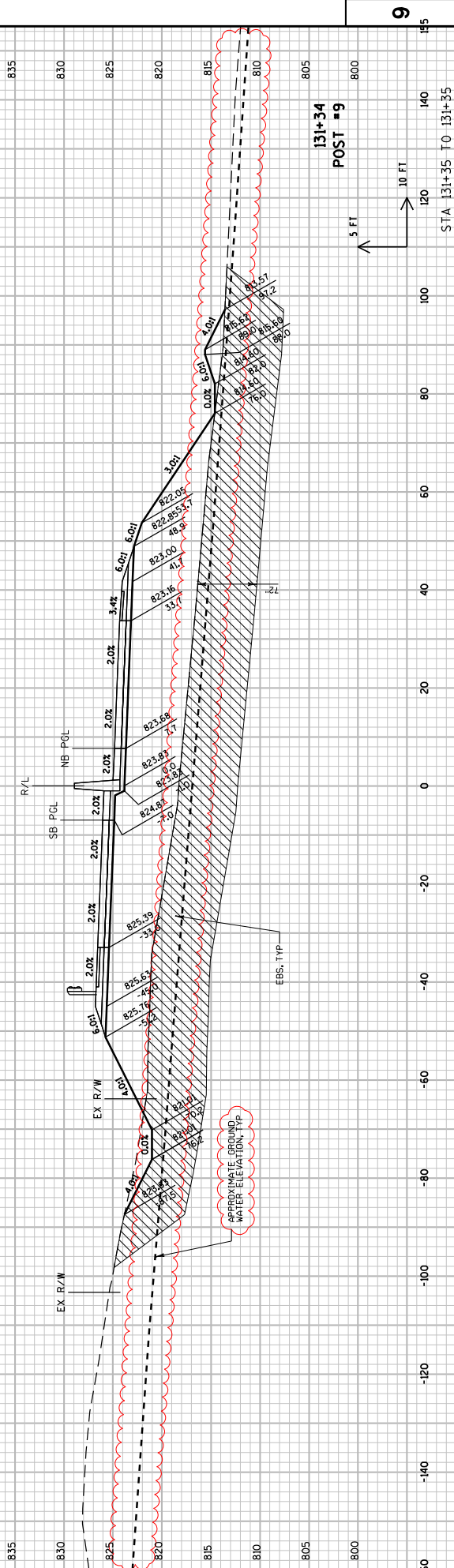
Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1496  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A



Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1497  
July 2, 2018

ALL WORK COMPLETED IN STAGE 2A

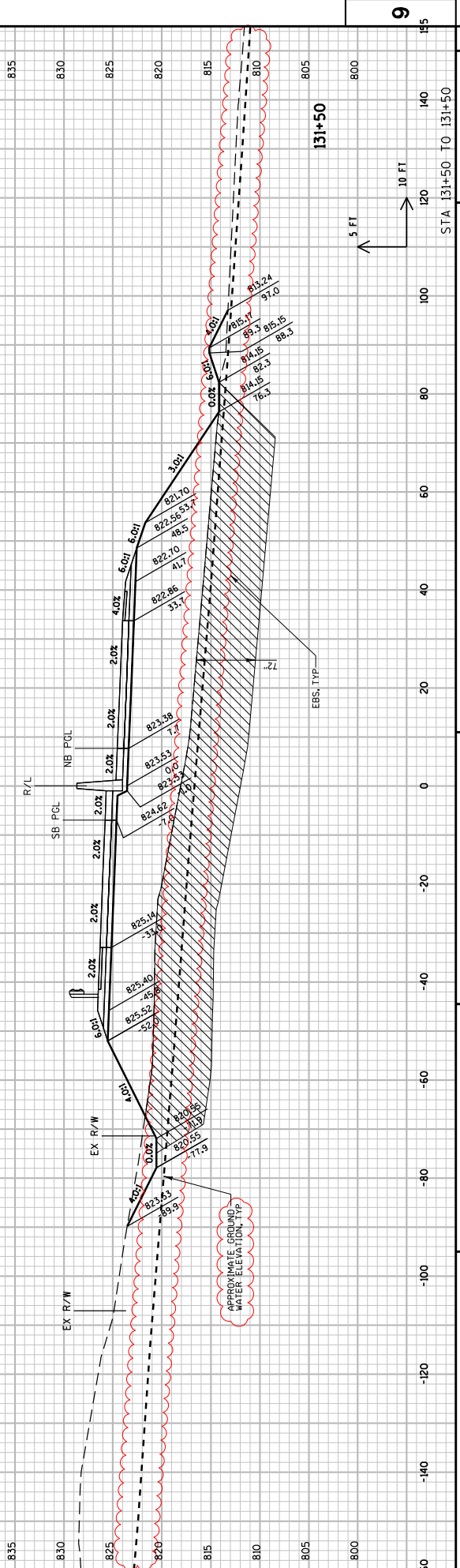


PROJECT NO: 2788-00-71  
COUNTY: WAUKESHA  
CROSS SECTIONS: WAUKESHA BYPASS  
SHEET 1497  
E

FILE NAME : \\wplw00\lmgpr\proj\59971\1\cadd\by\_090201\_xs\_wb.dgn  
PLOT DATE : 6/26/2018 10:25:09 AM  
PLOT BY : TFW.LP@lm  
PLOT SCALE : 20:1  
WISDOT/CADD SHEET 21

ALL WORK COMPLETED IN STAGE 2A

Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1498  
 July 2, 2018



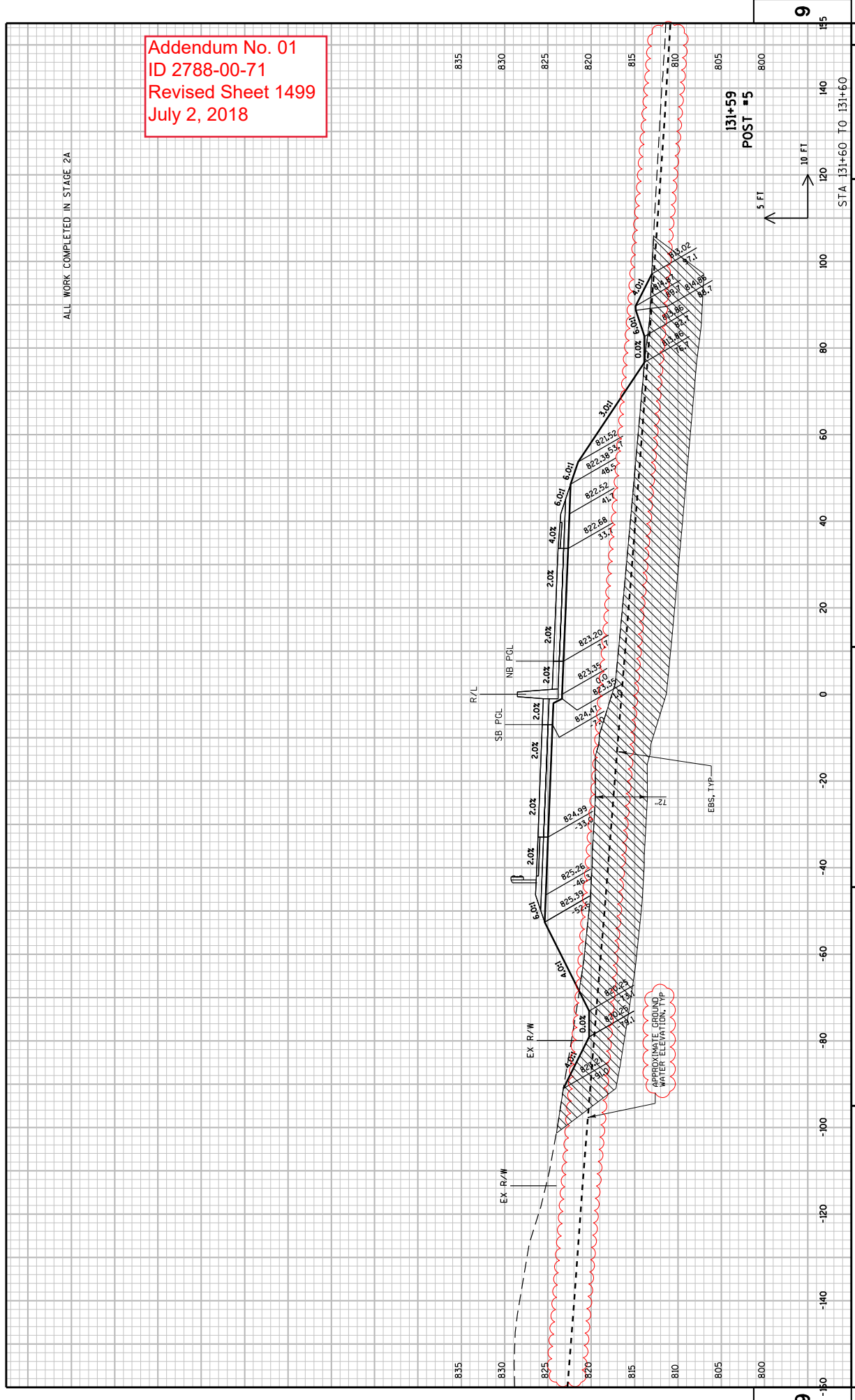
**PROJECT NO: 2788-00-71** **COUNTY: WAUKESHA** **CROSS SECTIONS: WAUKESHA BYPASS** **SHEET 1498**

FILE NAME : \\nw11w00\lmp\proj\59971\1\cadd\by\_090201\_xs\_mb.dgn PLOT DATE : 6/26/2018 10:25:09 AM PLOT BY : TFW.LP@ihr PLOT SCALE : 20:1

WISDOT/CADD SHEET 21

Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1499  
 July 2, 2018

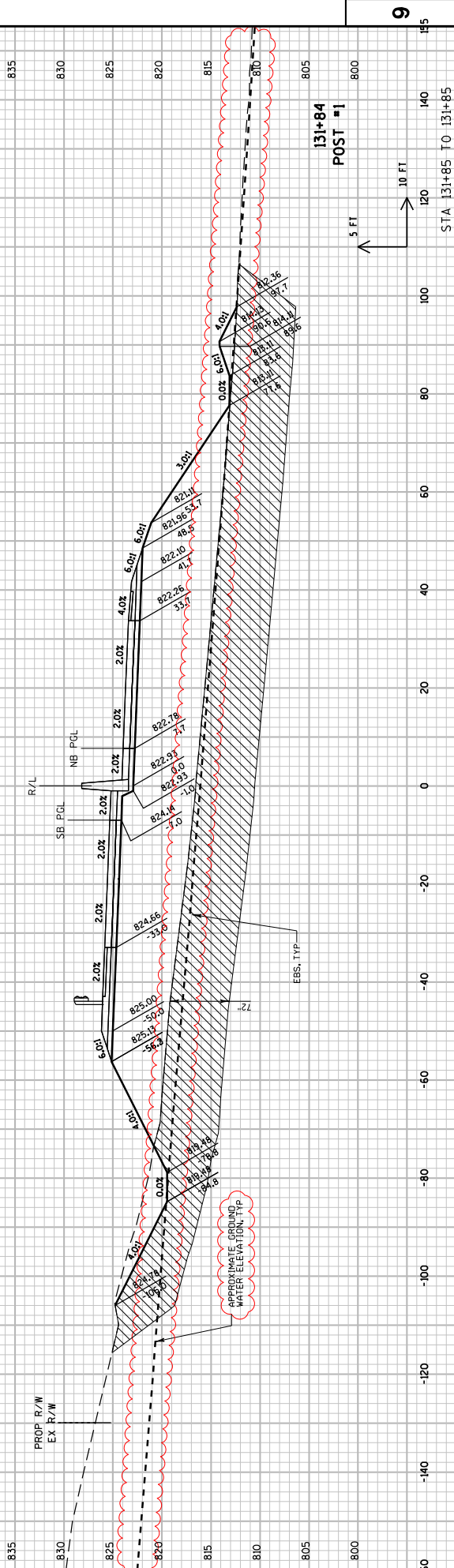
ALL WORK COMPLETED IN STAGE 2A





Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1500  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A



9

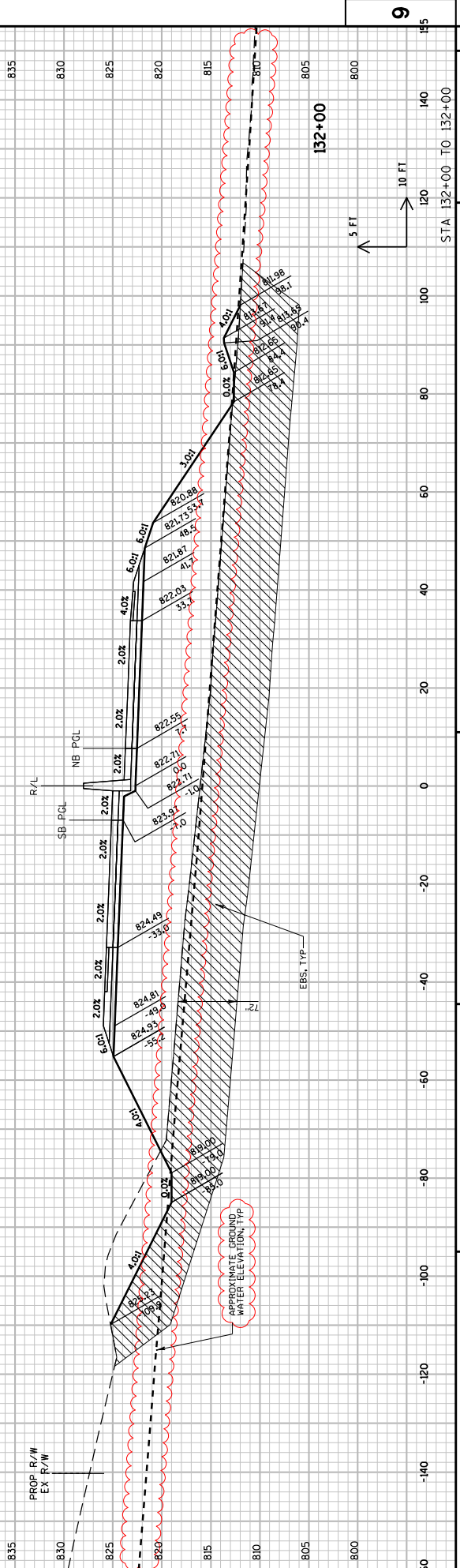
9

PROJECT NO: 2788-00-71  
 COUNTY: WAUKESHA  
 HWY: WAUKESHA BYPASS  
 CROSS SECTIONS: WAUKESHA BYPASS  
 SHEET 1500  
 STA 131+85 TO 131+85  
 POST #1  
 131+84

FILE NAME : \\nw11w00\lmp\proj\59971\1\cadd\by\_090201\_xs\_mb.dgn  
 PLOT DATE : 6/26/2018 10:25:12 AM  
 PLOT BY : TWA.LP@18  
 PLOT SCALE : 20:1  
 WISDOT/CADD SHEET 21

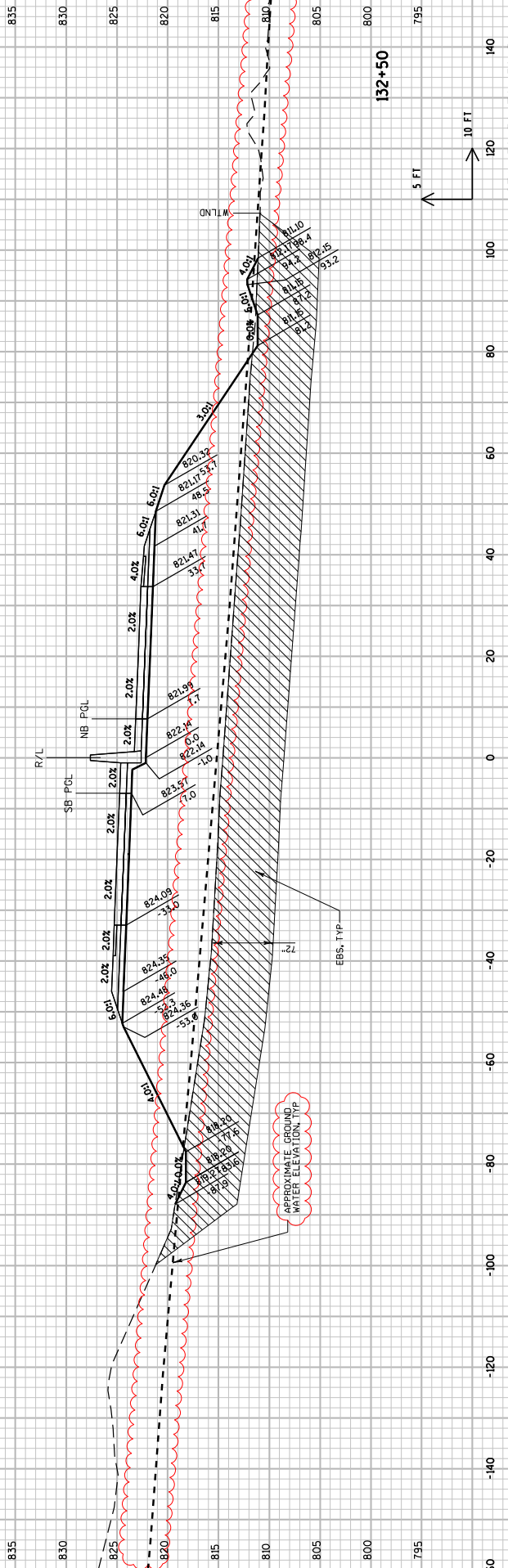
Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1501  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A



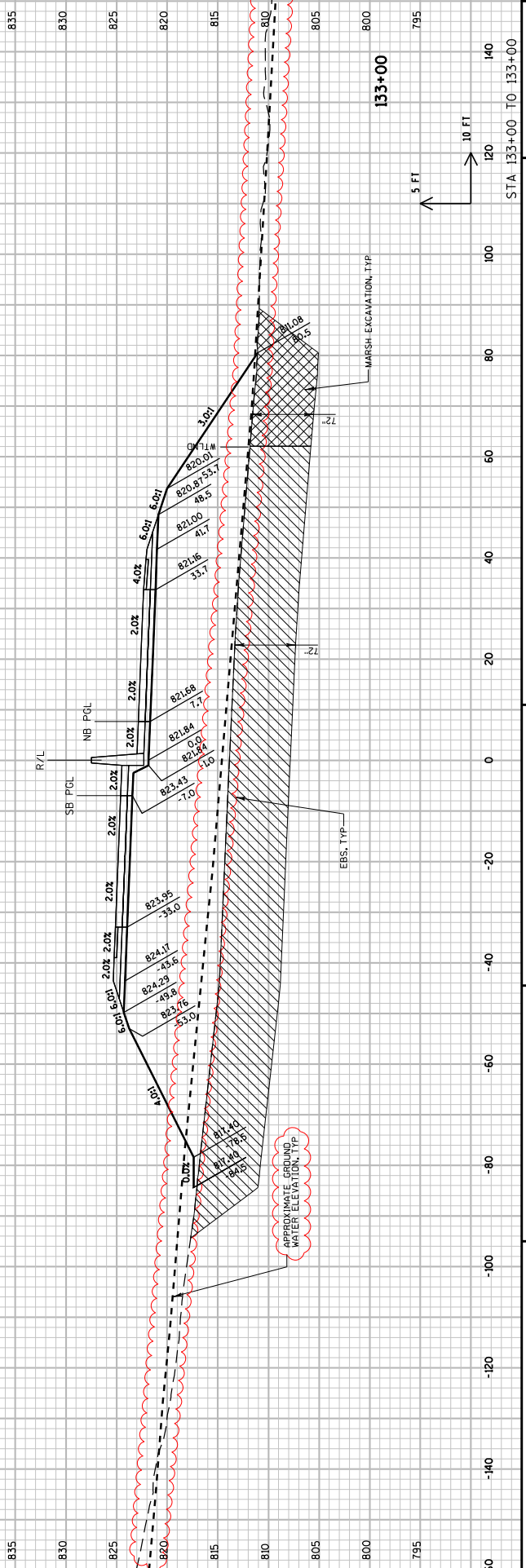
Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1502  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A



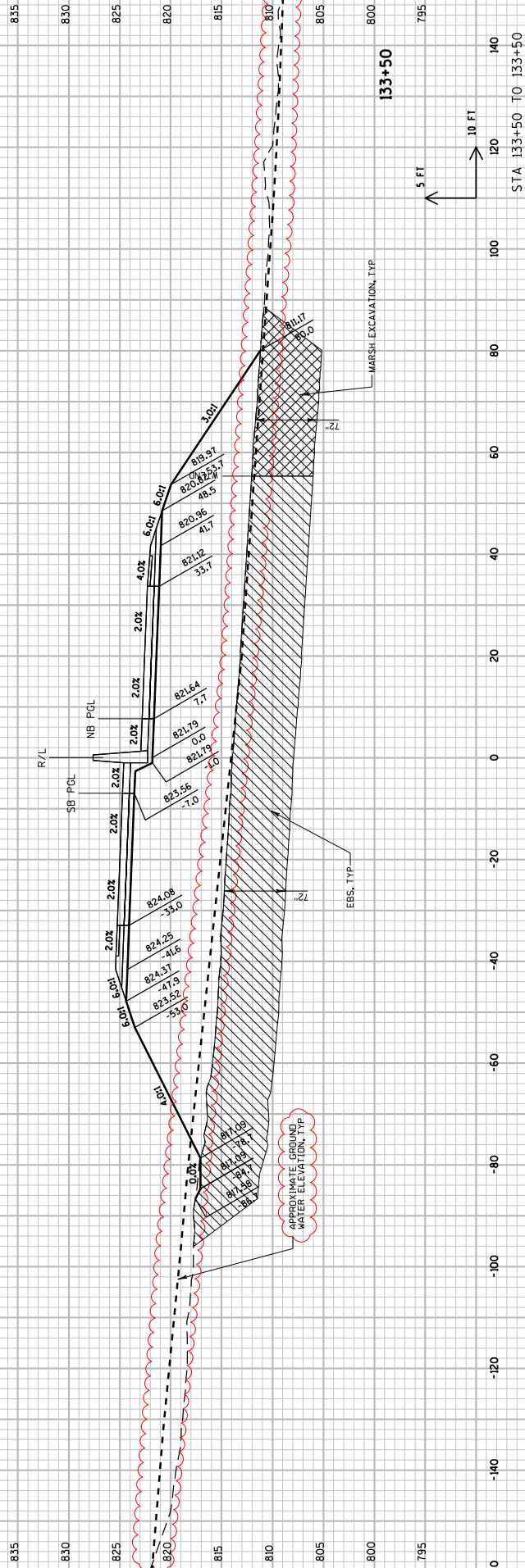
Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1503  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A



Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1504  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A

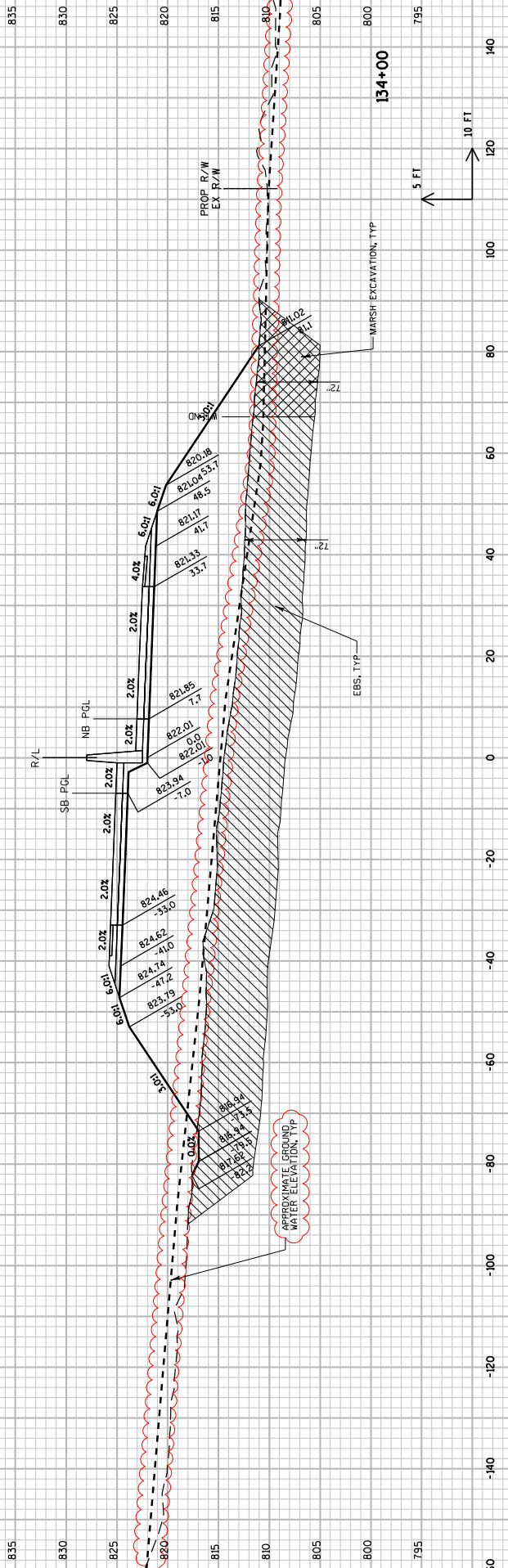


PROJECT NO: 2788-00-71 COUNTY: WAUKESHA CROSS SECTIONS: WAUKESHA BYPASS SHEET 1504

FILE NAME : \\wplw00\lmp\proj\59971\1\cadd\by\_090201\_xs.mb.dgn PLOT DATE : 6/26/2018 10:25:16 AM PLOT BY : TFW.LP@ihr PLOT SCALE : 20:1 WISDOT/CADD SHEET 21

Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1505  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A

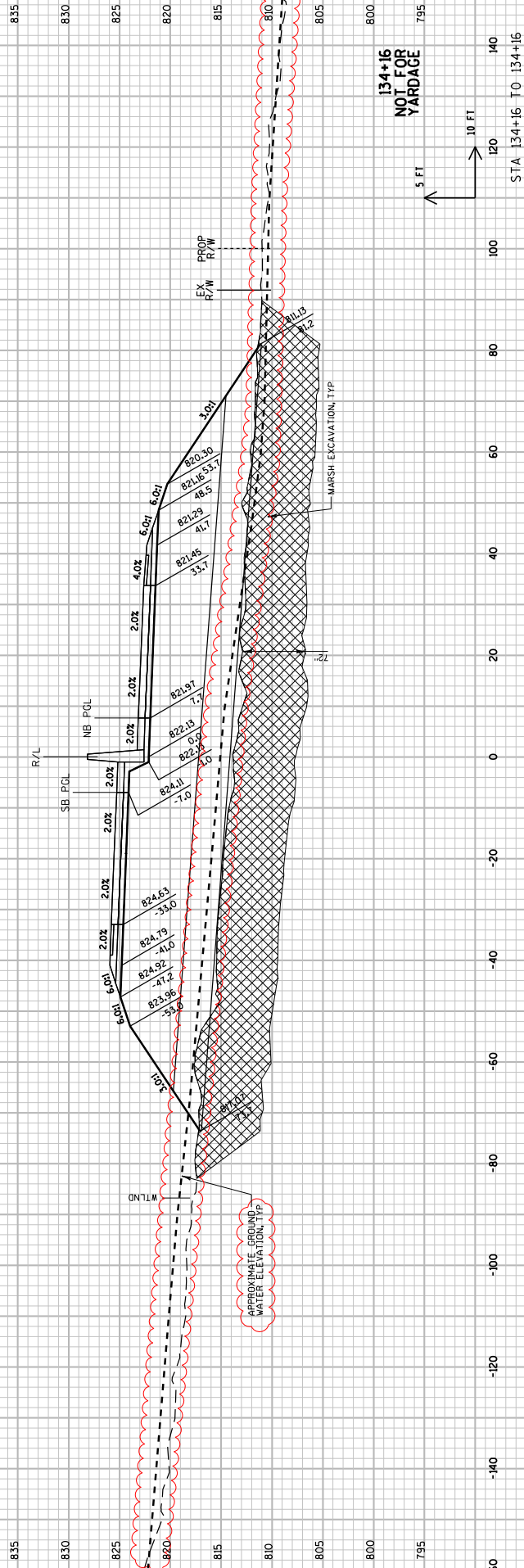


PROJECT NO: 2788-00-71  
 COUNTY: WAUKESHA  
 HWY: WAUKESHA BYPASS  
 CROSS SECTIONS: WAUKESHA BYPASS  
 SHEET 1505  
 E

FILE NAME : \\wplw00\lmp\proj\59971\1\cadd\by\_090201.XS.WB.dgn  
 PLOT DATE : 6/26/2018 10:25:17 AM  
 PLOT BY : TFW.LP@ihr  
 PLOT NAME :  
 PLOT SCALE : 20:1  
 WISDOT/CADD SHEET 21

Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1506  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A



9

9

134+16  
 NOT FOR  
 YARDAGE

5 FT

10 FT

STA 134+16 TO 134+16

SHEET 1506

CROSS SECTIONS: WAUKESHA BYPASS

COUNTY: WAUKESHA

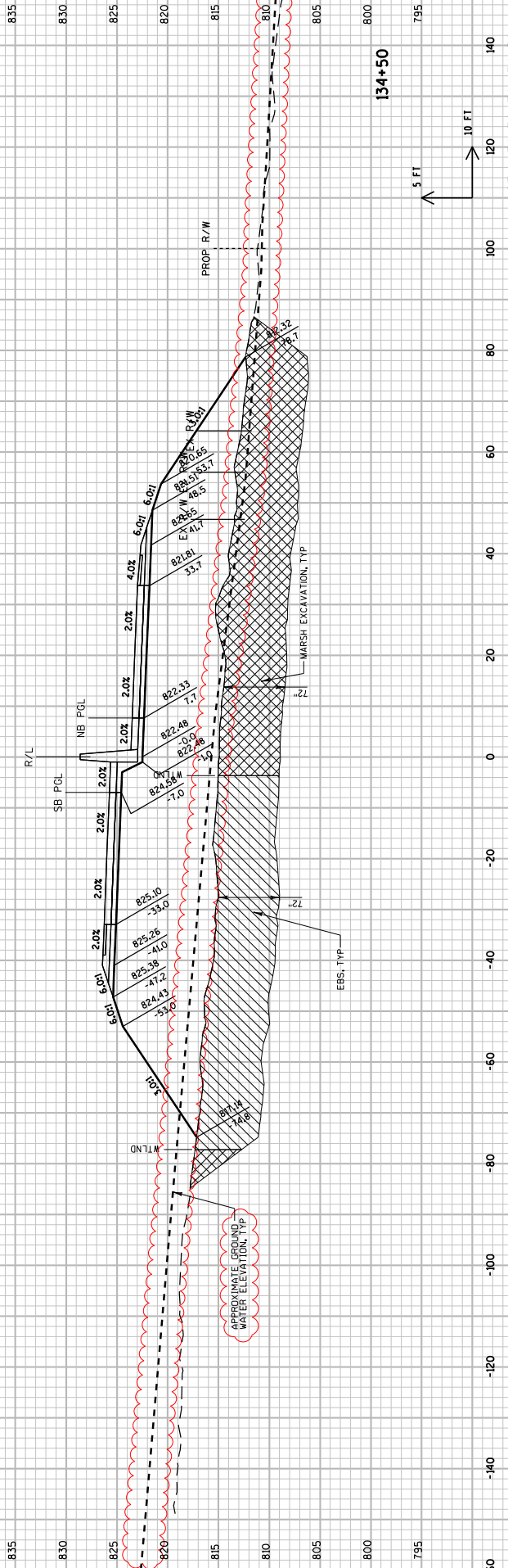
HWY: WAUKESHA BYPASS

PROJECT NO: 2788-00-71

FILE NAME : \\wplw00\lmp\proj\59971\1\cadd\by\_090201\_xs\_wb.dgn  
 PLOT DATE : 6/26/2018 10:25:18 AM  
 PLOT BY : fhw.lhp@lm  
 PLOT NAME :  
 PLOT SCALE : 20:1  
 WISDOT/CADD SHEET 21

Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1507  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A



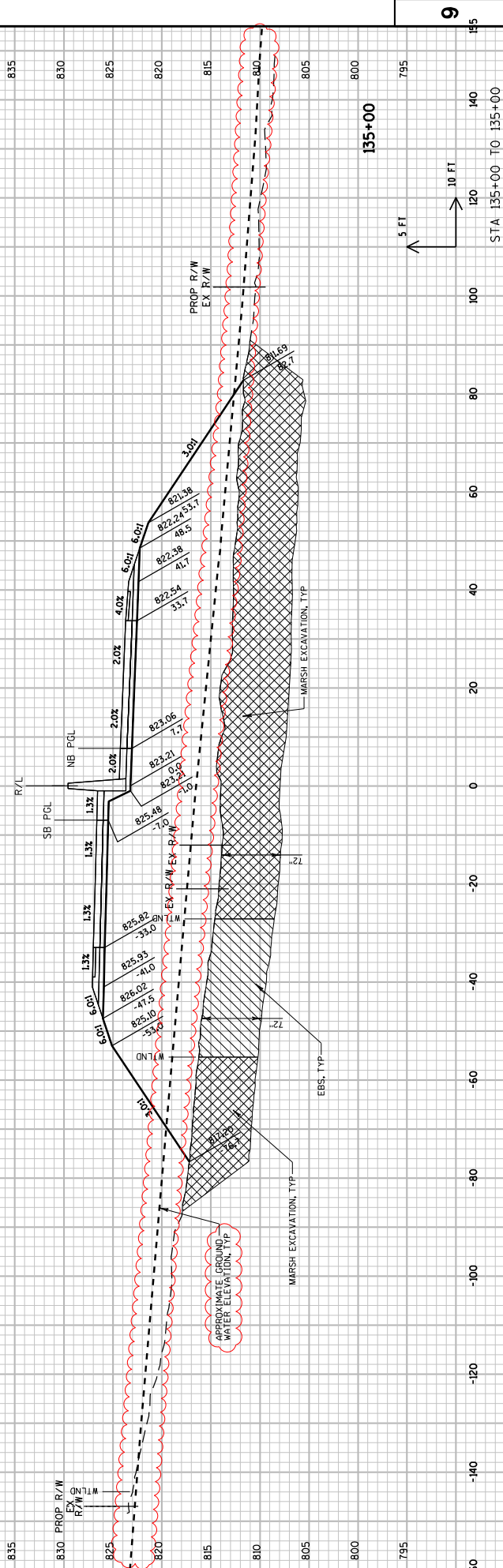
PROJECT NO: 2788-00-71      COUNTY: WAUKESHA      CROSS SECTIONS: WAUKESHA BYPASS      SHEET 1507

FILE NAME : \\nw11w00\lmp\proj\59971\1\cadd\by\_090201\_xs\_wb.dgn      PLOT DATE : 6/26/2018 10:25:19 AM      PLOT BY : Tm.L.Phe.lm      PLOT SCALE : 20:1      WISDOT/CADD SHEET 21



Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1508  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A



PROJECT NO: 2788-00-71  
 COUNTY: WAUKESHA  
 HWY: WAUKESHA BYPASS  
 CROSS SECTIONS: WAUKESHA BYPASS  
 SHEET 1508  
 E

ALL WORK COMPLETED IN STAGE 2A

Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1509  
July 2, 2018

840

835

830

825

820

815

810

805

800

795

R/L

SB PCL

0.1%

0.1%

0.1%

0.1%

0.1%

0.1%

0.1%

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PROP R/W  
EX R/W

EX R/W  
EX R/W

EX R/W  
EX R/W

EX R/W  
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EX R/W  
EX R/W

EX R/W  
EX R/W

EX R/W  
EX R/W

EX R/W  
EX R/W

EX R/W  
EX R/W

825  
33.0  
825.82  
-48.0  
825.87  
-48.0  
825.81  
-48.0  
825.86  
-41.0  
825.45  
-33.0  
825.46  
-33.0  
825.46  
-33.0

820  
33.1  
823.50  
41.1  
823.34  
48.5  
823.20  
53.1  
822.35  
48.5

815  
33.1  
824.02  
1.1  
824.17  
0.0  
824.17  
-1.0  
826.46  
-7.0

810  
33.1  
823.50  
41.1  
823.34  
48.5  
823.20  
53.1  
822.35  
48.5

805  
33.1  
823.50  
41.1  
823.34  
48.5  
823.20  
53.1  
822.35  
48.5

800  
33.1  
823.50  
41.1  
823.34  
48.5  
823.20  
53.1  
822.35  
48.5

795  
33.1  
823.50  
41.1  
823.34  
48.5  
823.20  
53.1  
822.35  
48.5

790  
33.1  
823.50  
41.1  
823.34  
48.5  
823.20  
53.1  
822.35  
48.5

785  
33.1  
823.50  
41.1  
823.34  
48.5  
823.20  
53.1  
822.35  
48.5

825

820

815

810

805

800

795

790

785

APPROXIMATE GROUND  
WATER ELEVATION, TYP

EBS, TYP

MARKSH EXCAVATION, TYP

EBS, TYP

135+50

805

825

820

815

810

805

800

795

790

785

135+50

140

145

150

155

160

165

170

175

5 FT

10 FT

STA 135+50 TO 135+50

9

E

PROJECT NO: 2788-00-71

HWY: WAUKESHA BYPASS

COUNTY: WAUKESHA

CROSS SECTIONS: WAUKESHA BYPASS

SHEET 1509

E

FILE NAME : \\\wpl\1\001\pgr\proj\159971\1\ccds\wy\_090201.XS.MB.dgn PLOT DATE : 6/26/2018 10:25:30 AM PLOT BY : FW.LP@IPIH PLOT SCALE : 20:1 WISDOT/CADD SHEET 21

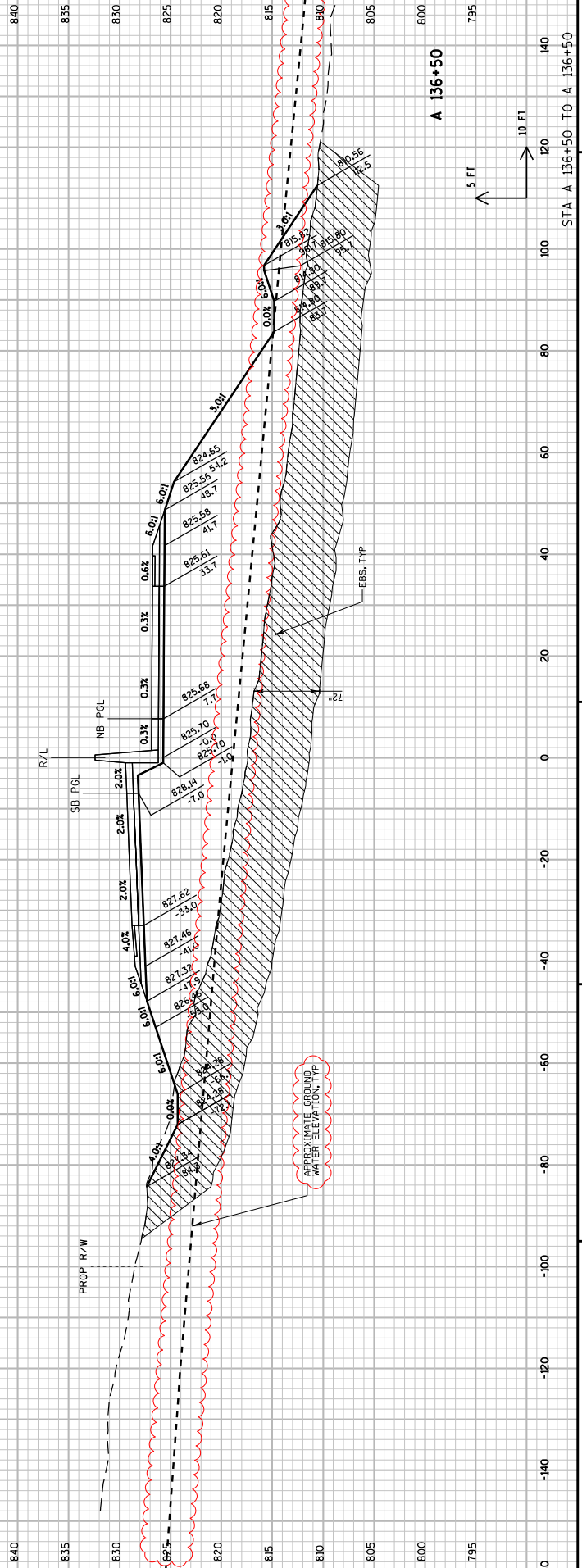
Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1510  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A



Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1511  
July 2, 2018

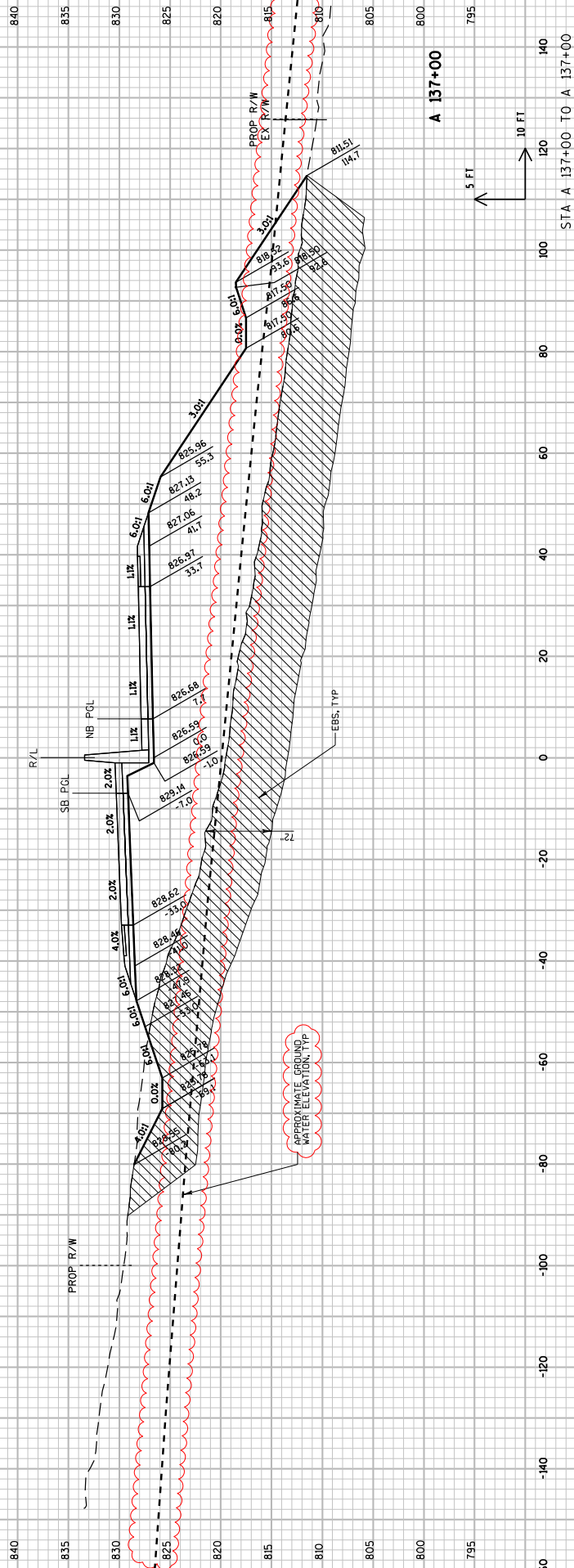
ALL WORK COMPLETED IN STAGE 2A



|                                                                  |                  |                                 |            |
|------------------------------------------------------------------|------------------|---------------------------------|------------|
| PROJECT NO: 2788-00-71                                           | COUNTY: WAUKESHA | CROSS SECTIONS: WAUKESHA BYPASS | SHEET 1511 |
| HWY: WAUKESHA BYPASS                                             |                  | STA A 136+50 TO A 136+50        |            |
| FILE NAME : \\nw11w00\jmgr\proj\59971\1\cadd\by_090201_xs_wb.dgn |                  | PLOT SCALE : 20:1               |            |
| PLOT DATE : 6/26/2018 10:25:33 AM                                |                  | PLOT BY : TWL\LPB\JM            |            |
| PLOT NAME :                                                      |                  | WISDOT/CADD SHEET 21            |            |

Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1512  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A



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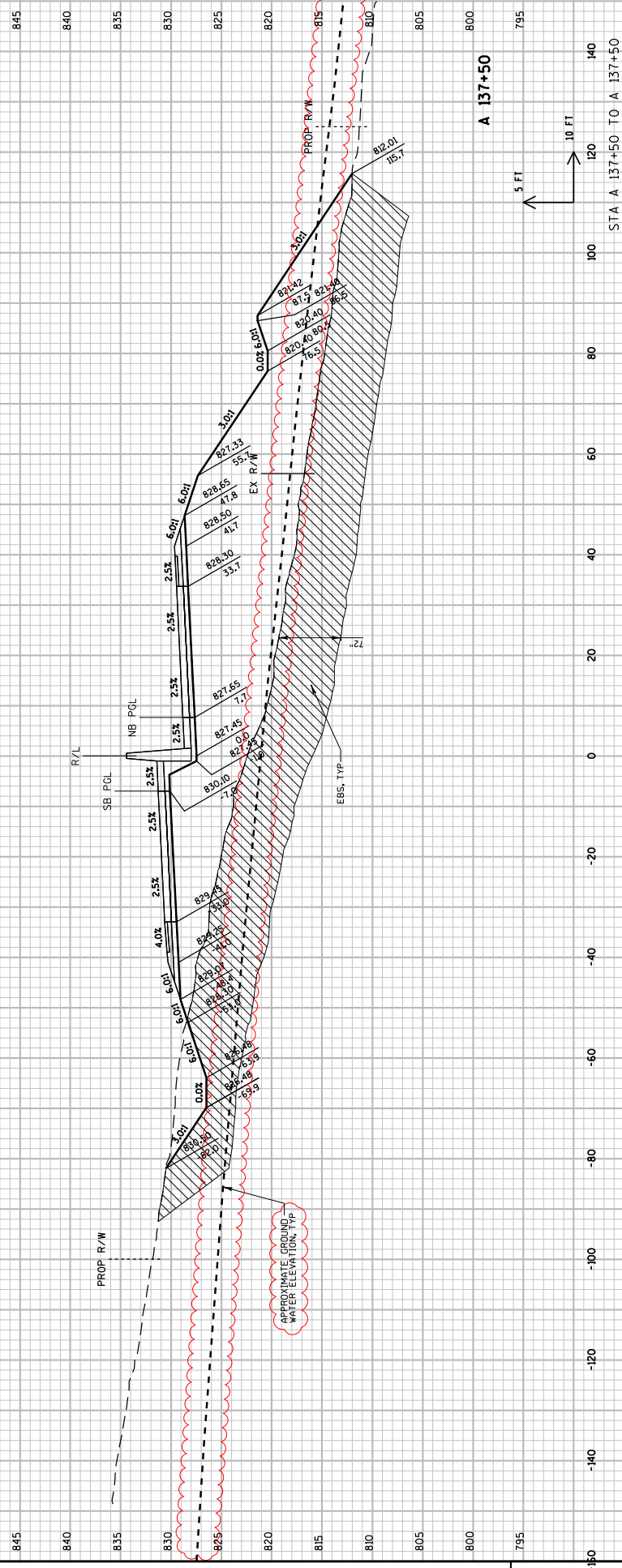
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PROJECT NO: 2788-00-71  
 COUNTY: WAUKESHA  
 HWY: WAUKESHA BYPASS  
 CROSS SECTIONS: WAUKESHA BYPASS  
 SHEET 1512  
 STA. A 137+00 TO A 137+00

FILE NAME : \\nw11w00\lmp\proj\59971\1\cadd\by\_090201\_xs\_wb.dgn  
 PLOT BY : TFW.LP@ihr  
 PLOT DATE : 6/26/2018 10:25:33 AM  
 PLOT SCALE : 20:1  
 WISDOT/CADD SHEET 21

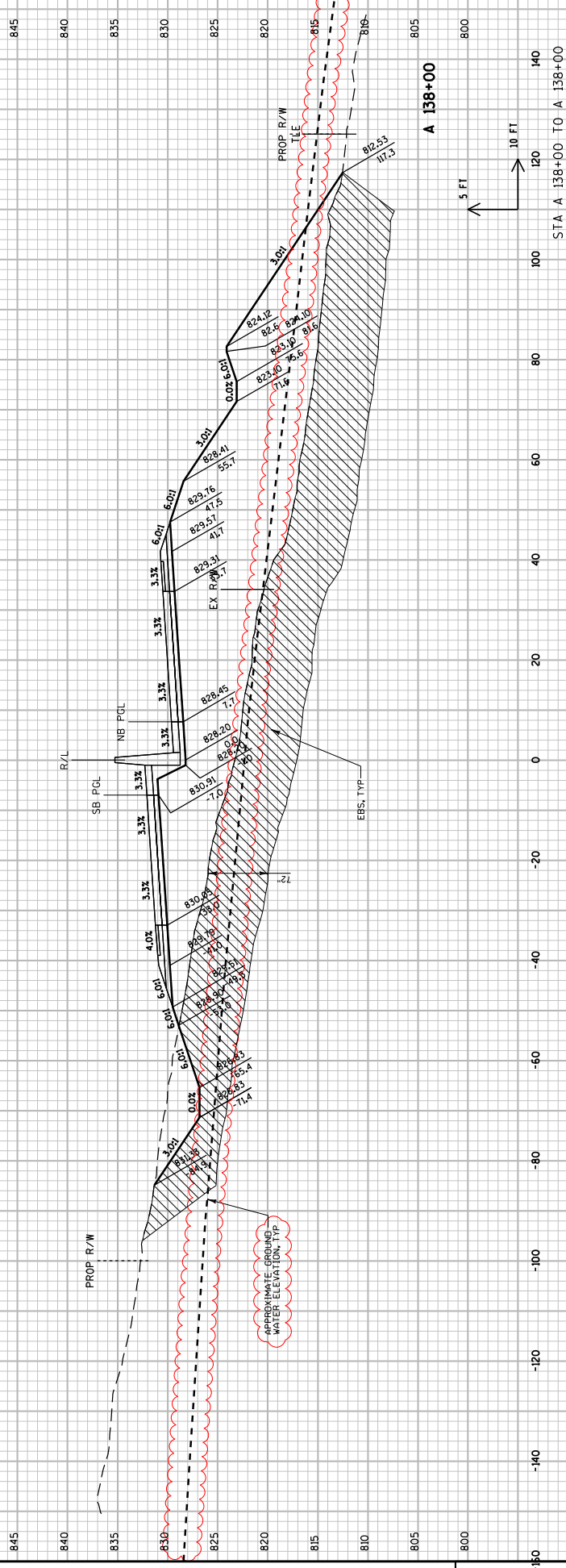
ALL WORK COMPLETED IN STAGE 2A

Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1513  
 July 2, 2018



Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1514  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A

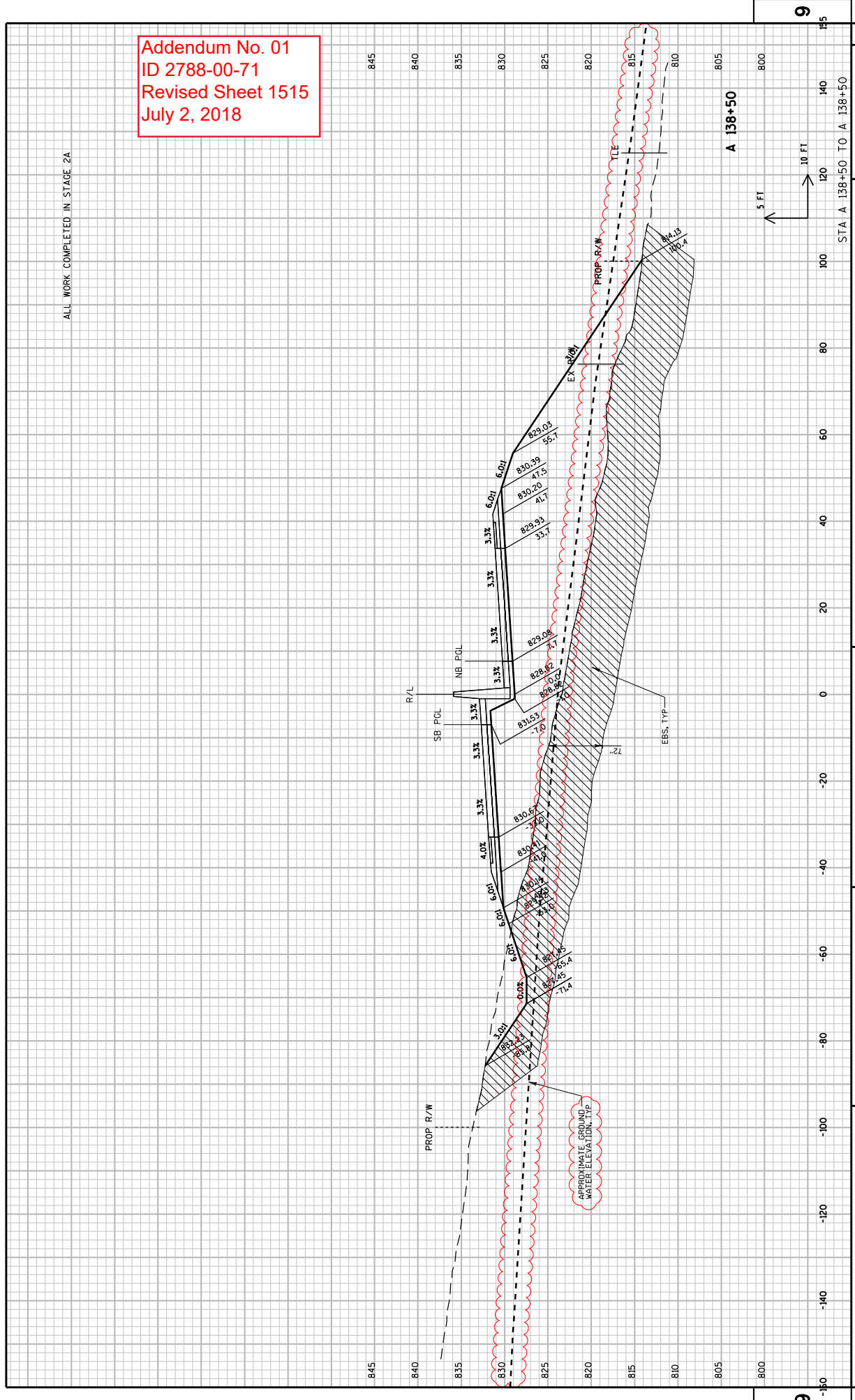


PROJECT NO: 2788-00-71  
 COUNTY: WAUKESHA  
 HWY: WAUKESHA BYPASS  
 CROSS SECTIONS: WAUKESHA BYPASS  
 SHEET 1514  
 E

FILE NAME : \\nw11w00\lmgpr\proj\59971\1\cadd\by\_090201\_xs\_wb.dgn  
 PLOT DATE : 6/26/2018 10:25:36 AM  
 PLOT BY : TFW.LPH:JRM  
 PLOT SCALE : 20:1

Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1515  
 July 2, 2018

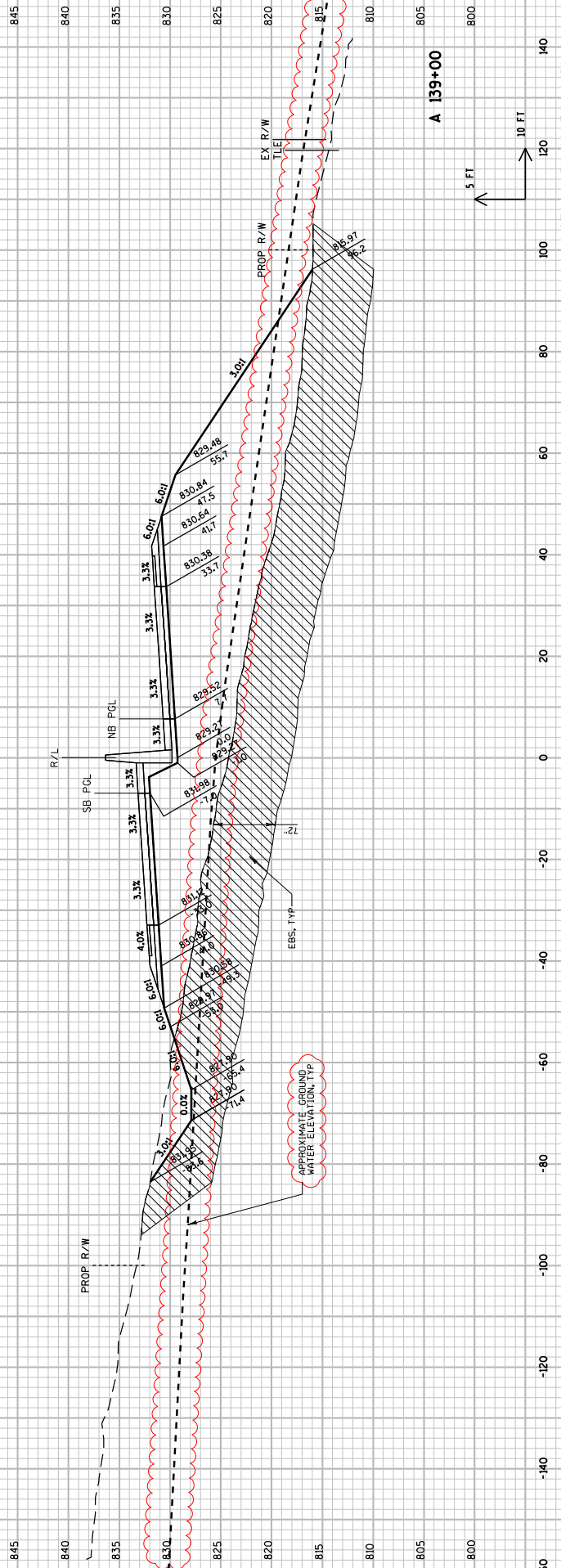
ALL WORK COMPLETED IN STAGE 2A





ALL WORK COMPLETED IN STAGE 2A

Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1516  
July 2, 2018



10 FT  
5 FT

A 139+00

9

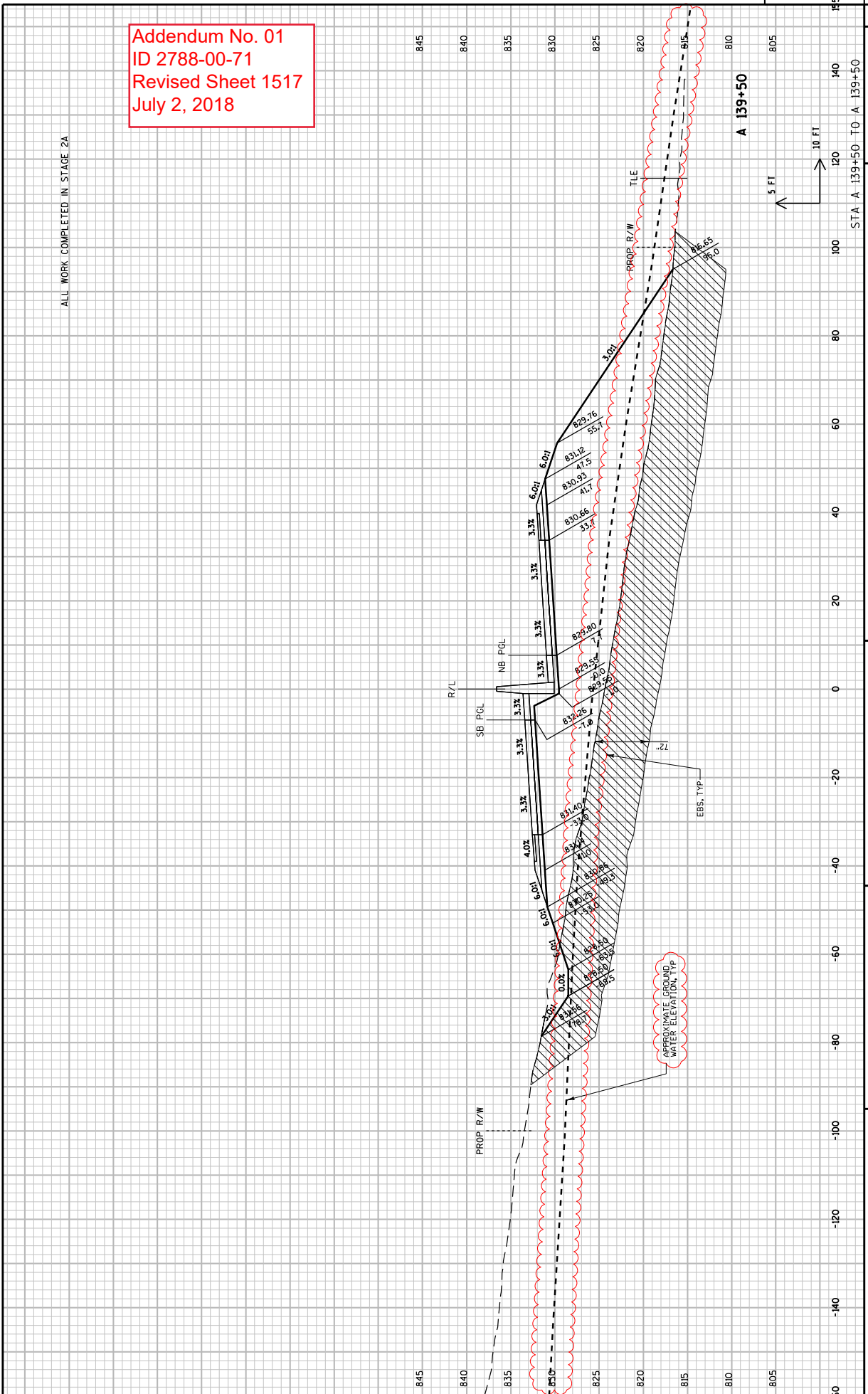
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PROJECT NO: 2788-00-71  
COUNTY: WAUKESHA  
HWY: WAUKESHA BYPASS  
CROSS SECTIONS: WAUKESHA BYPASS  
SHEET 1516  
STA. A 139+00 TO A 139+00

FILE NAME : \\wplw00\lmp\proj\59971\1\cadd\by\_090201\_xs\_wb.dgn  
PLOT BY : TFW.LP@WI  
PLOT DATE : 6/26/2018 10:25:41 AM  
PLOT SCALE : 20:1  
WISDOT/CADD SHEET 21

Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1517  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A



CROSS SECTIONS: WAUKESHA BYPASS

COUNTY: WAUKESHA

HWY: WAUKESHA BYPASS

PROJECT NO: 2788-00-71

SHEET 1517

E

PLOT SCALE: 20:1

PLOT BY: TFW.LPH:JM

PLOT DATE: 6/26/2018 10:25:42 AM

FILE NAME: \\nw11w00\jngpr\proj\59971\1\cadd\sv\_090201\_xs\_nb.dgn

WISDOT/CADD SHEET 21

Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1518  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A

845

840

835

830

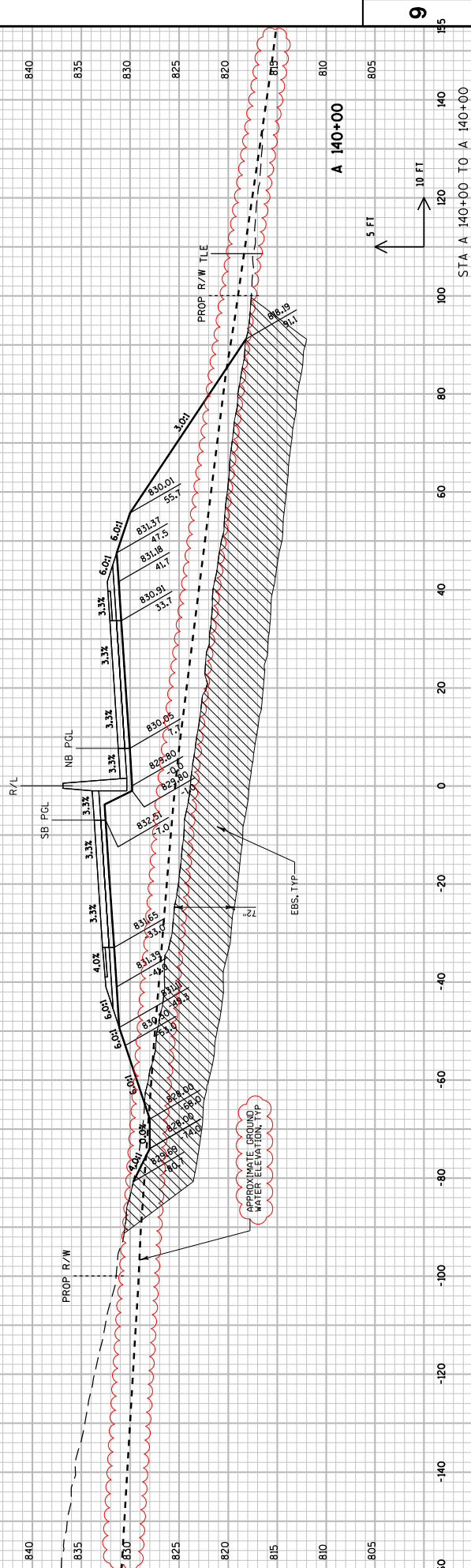
825

820

815

810

805



9

10 FT

5 FT

100

120

140

155

A 140+00

STA A 140+00 TO A 140+00

SHEET 1518

E

PROJECT NO: 2788-00-71

HWY: WAUKESHA BYPASS

COUNTY: WAUKESHA

CROSS SECTIONS: WAUKESHA BYPASS

PLOT NAME :

PLOT BY : TWI.LPH:JM

PLOT DATE : 6/26/2018 10:25:43 AM

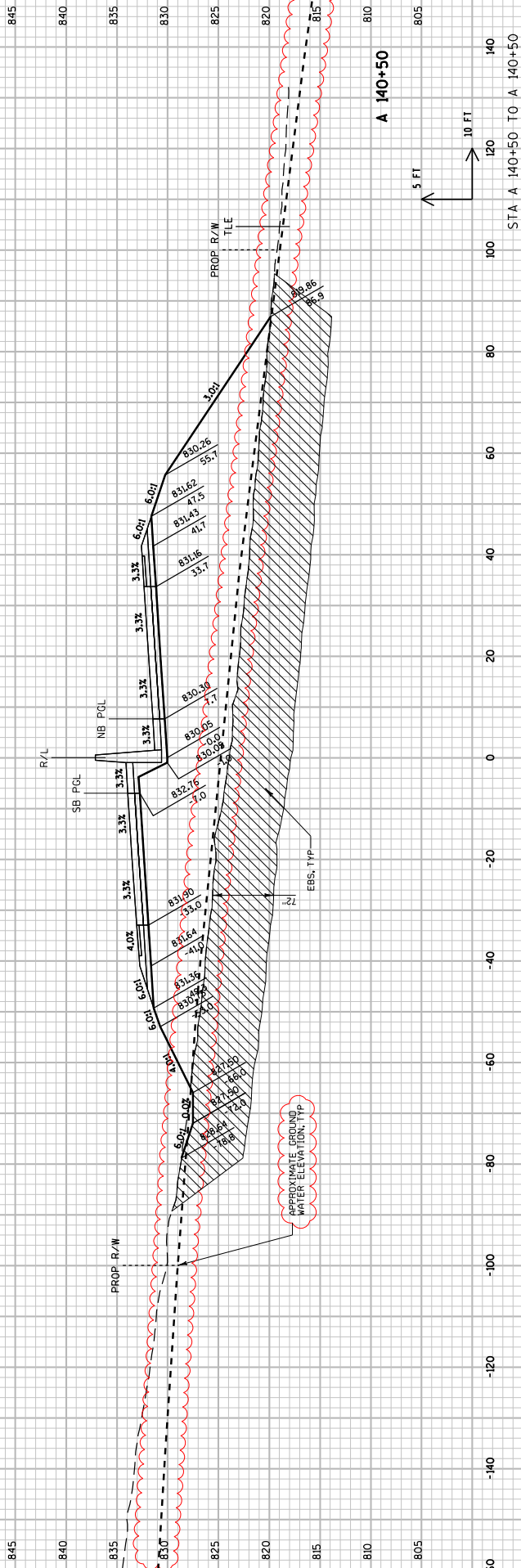
PLOT SCALE : 20:1

WISDOT/CADD SHEET 21

FILE NAME : \\nw11w00\jmgr\proj\59971\1\cadd\by\_090201\_xs\_wb.dgn

ALL WORK COMPLETED IN STAGE 2A

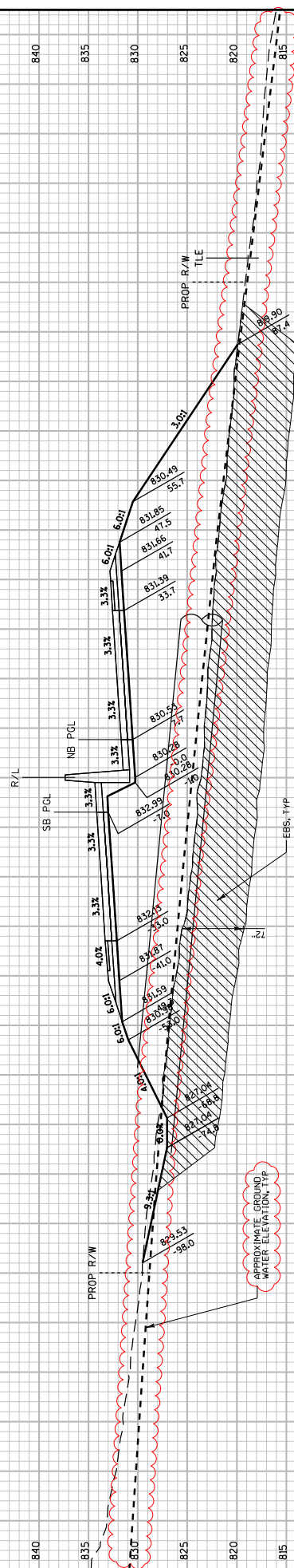
Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1519  
July 2, 2018



ALL WORK COMPLETED IN STAGE 2A

Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1520  
July 2, 2018

845  
840  
835  
830  
825  
820  
815  
810  
805



A 140+96  
NOT FOR  
YARDAGE

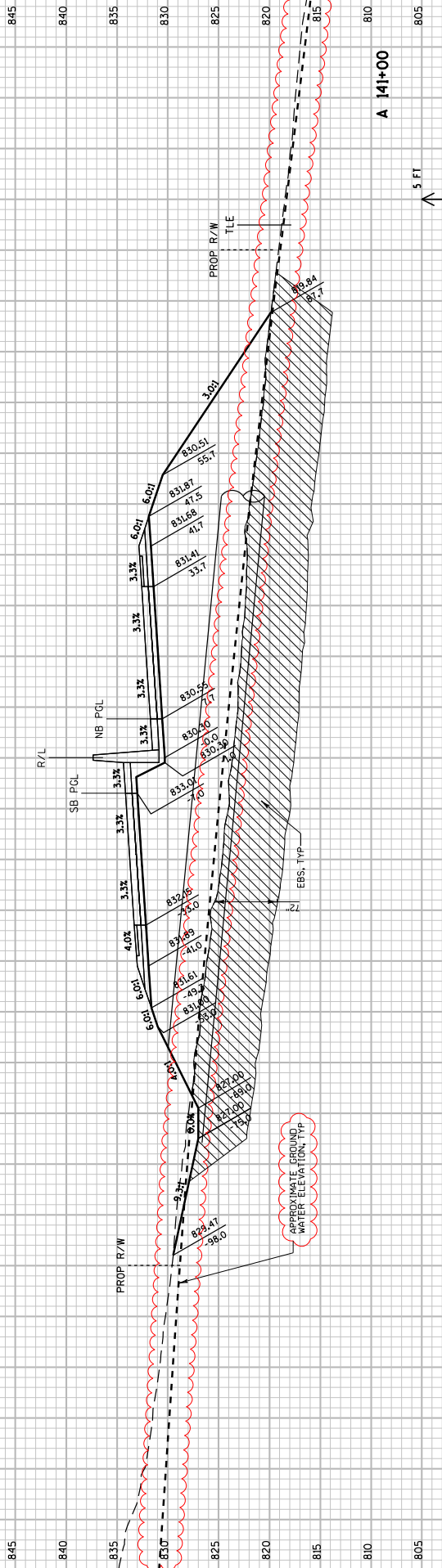
5 FT  
10 FT

9  
150 140 120 100 80 60 40 20 0 -20 -40 -60 -80 -100 -120 -140 -150  
STA A 140+96 TO A 140+96  
SHEET 1520  
E

PROJECT NO: 2788-00-71  
COUNTY: WAUKESHA  
HWY: WAUKESHA BYPASS  
CROSS SECTIONS: WAUKESHA BYPASS  
PLOT NAME :  
PLOT DATE : 6/26/2018 10:25:46 AM  
PLOT BY : TFW.L.PHE.JR  
WISDOT/CADD SHEET 21

ALL WORK COMPLETED IN STAGE 2A

Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1521  
July 2, 2018



845  
840  
835  
830  
825  
820  
815  
810  
805

9

10 FT  
5 FT

STA A 141+00 TO A 141+00

100  
120  
140  
155

A 141+00

100  
120  
140  
155

9

PROJECT NO: 2788-00-71

COUNTY: WAUKESHA

CROSS SECTIONS: WAUKESHA BYPASS

SHEET 1521

E

FILE NAME : \\wplw00\lmgpr\proj\59971\1\cadd\by\_090201\_xs\_wb.dgn

PLOT DATE : 6/26/2018 10:25:47 AM

PLOT BY : TFW.LP@wplw.com

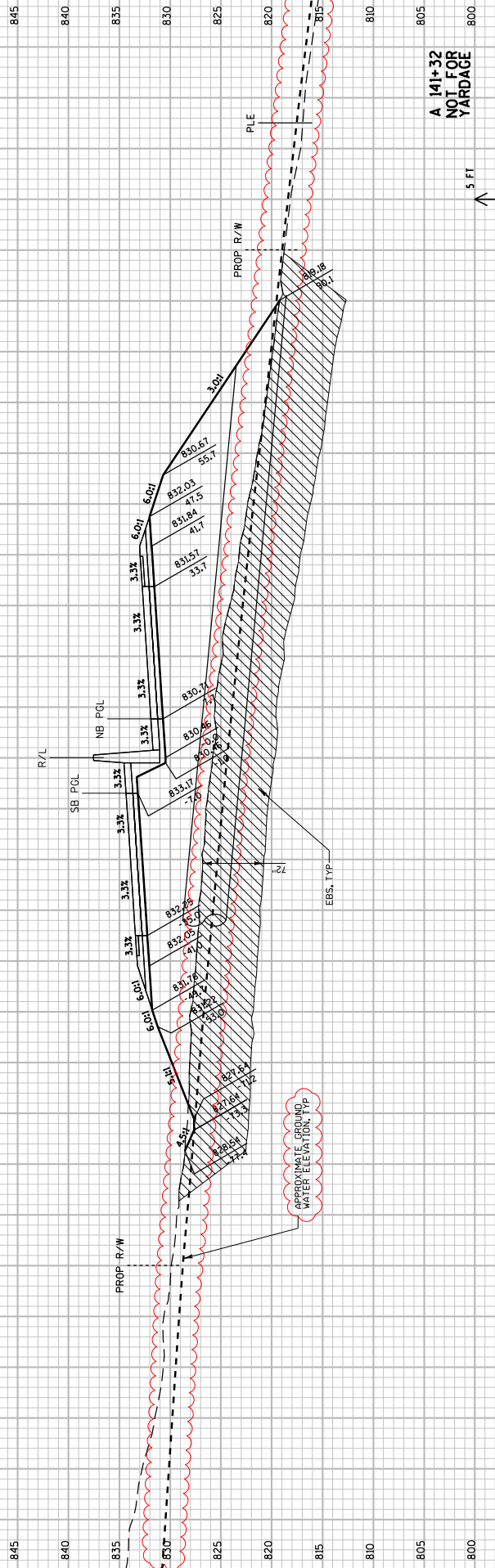
PLOT NAME :

PLOT SCALE : 20:1

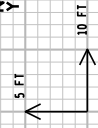
WISDOT/CADD SHEET 21

ALL WORK COMPLETED IN STAGE 2A

Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1522  
 July 2, 2018



A 141+32  
 NOT FOR  
 YARDAGE



845  
840  
835  
830  
825  
820  
815  
810  
805  
800

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10 FT  
5 FT

STA A 141+32 TO A 141+32

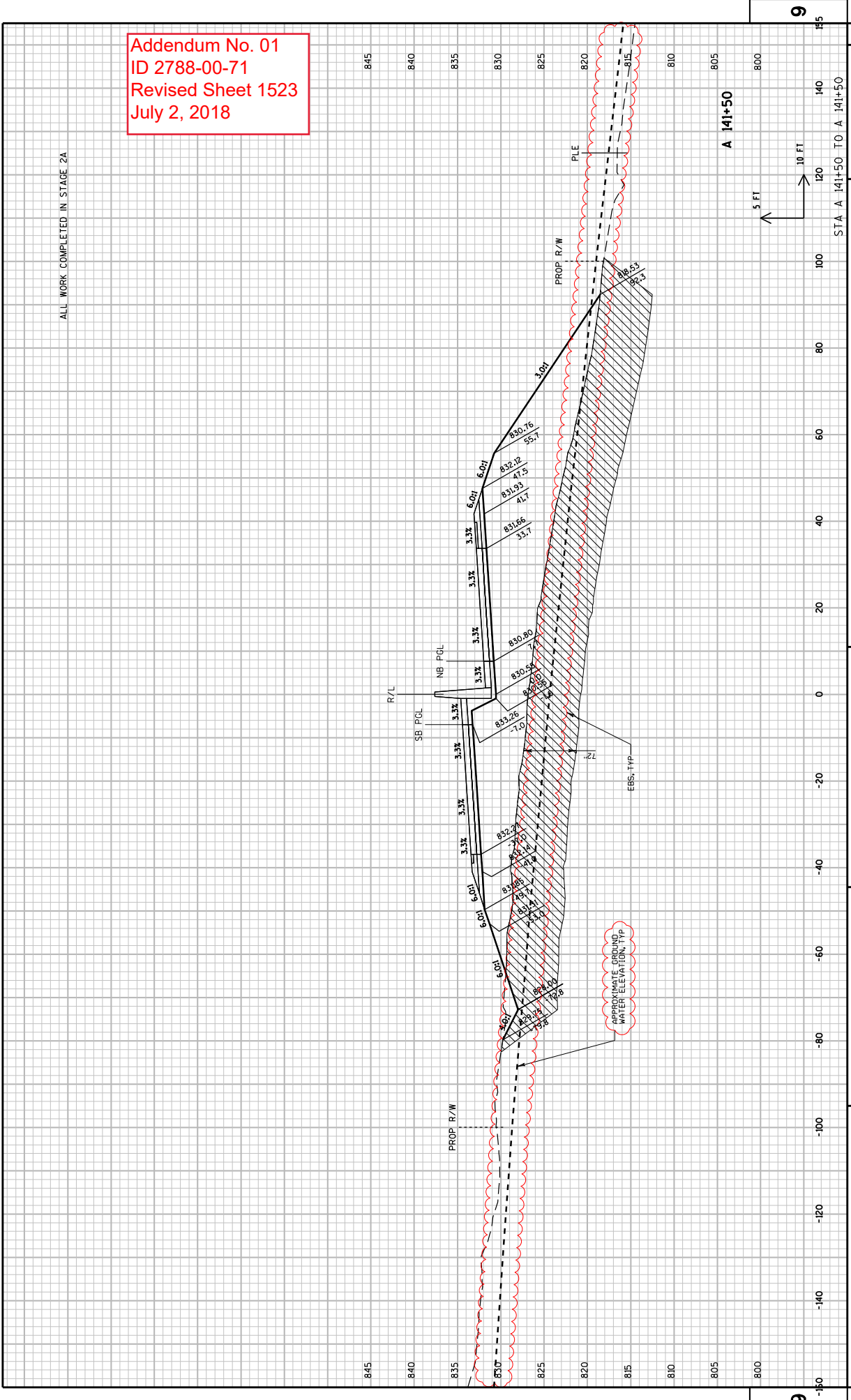
100  
120  
140  
150

PROJECT NO: 2788-00-71  
 COUNTY: WAUKESHA  
 CROSS SECTIONS: WAUKESHA BYPASS  
 SHEET 1522

FILE NAME : \\wplw00\lmgpr\proj\59971\1\ccds\by\_090201\_xs\_wb.dgn  
 PLOT DATE : 6/26/2018 10:25:49 AM  
 PLOT BY : TWJLPHJH  
 PLOT SCALE : 20:1  
 WISDOT/CADD SHEET 21

Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1523  
July 2, 2018

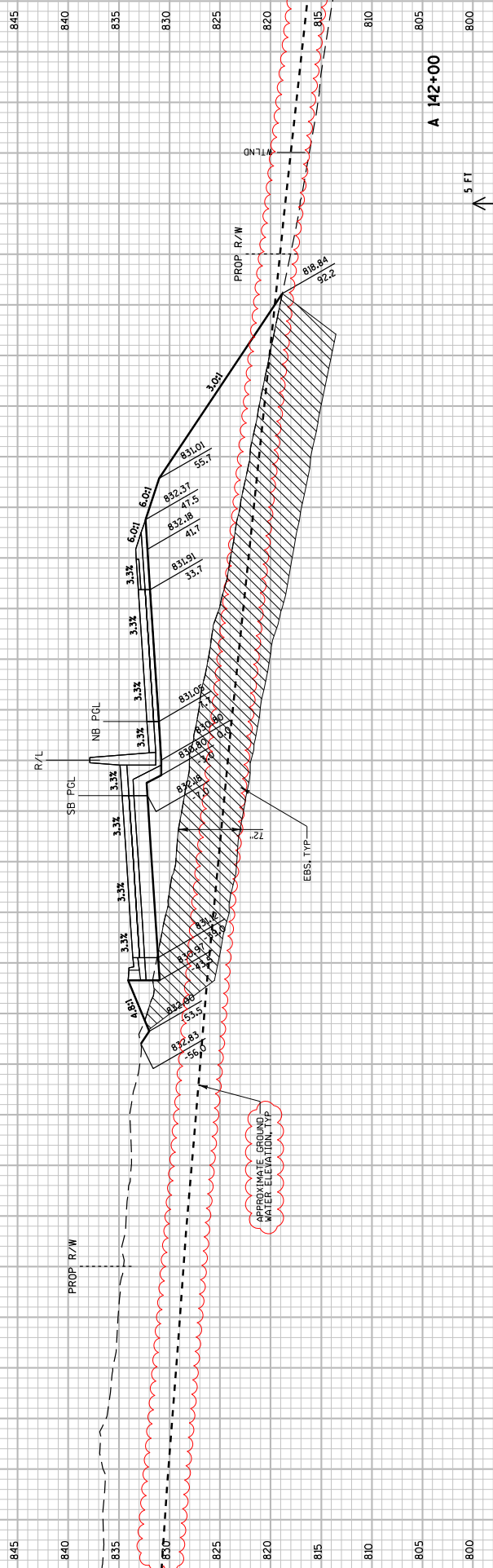
ALL WORK COMPLETED IN STAGE 2A





Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1524  
 July 2, 2018

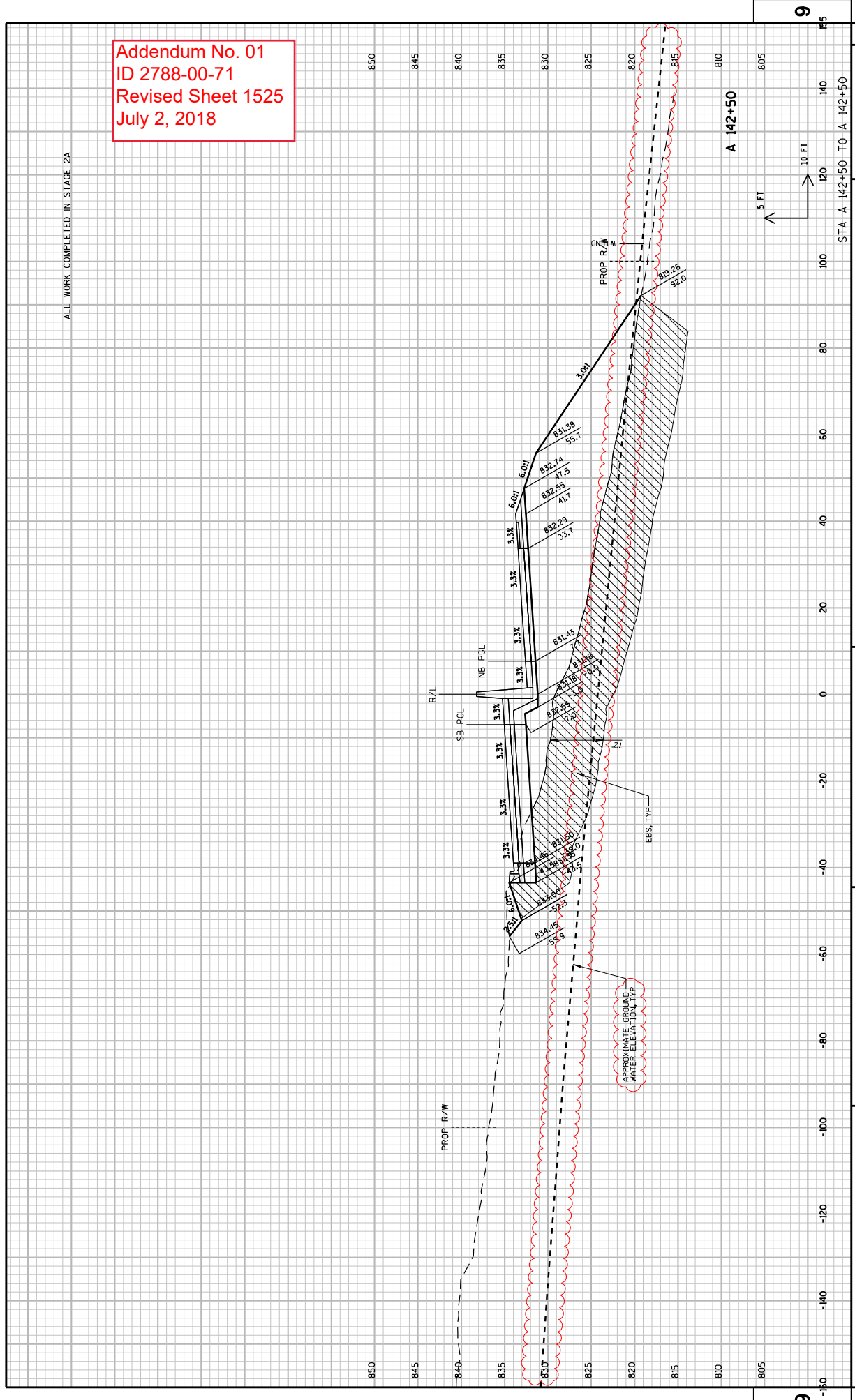
ALL WORK COMPLETED IN STAGE 2A



9  
 150 140 130 120 110 100 90 80 70 60 50 40 30 20 10 0 -10 -20 -30 -40 -50 -60 -70 -80 -90 -100 -110 -120 -130 -140 -150  
 STA. A 142+00 TO A 142+00  
 10 FT  
 5 FT  
 A 142+00  
 800 805 810 815 820 825 830 835 840 845

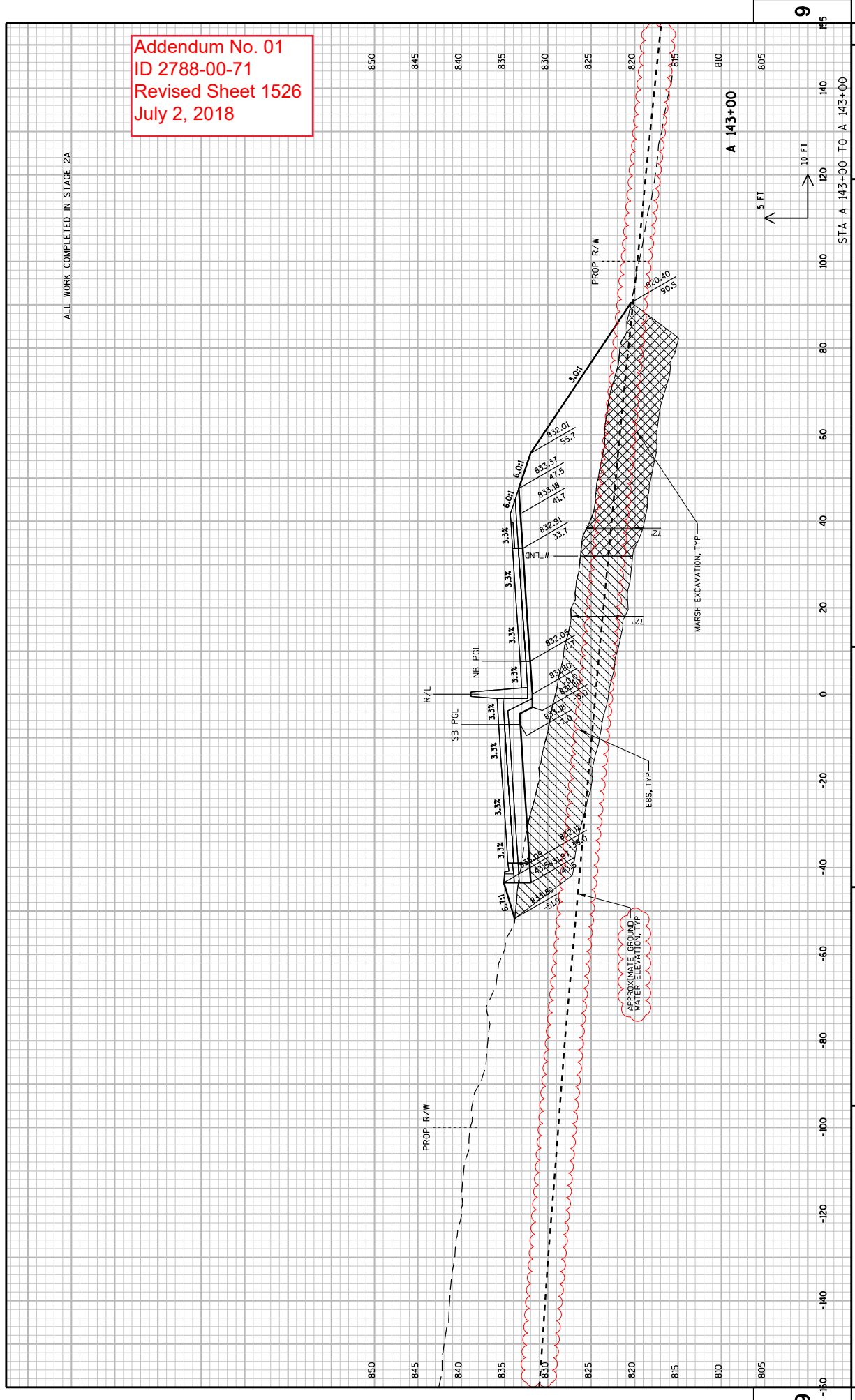
Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1525  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A



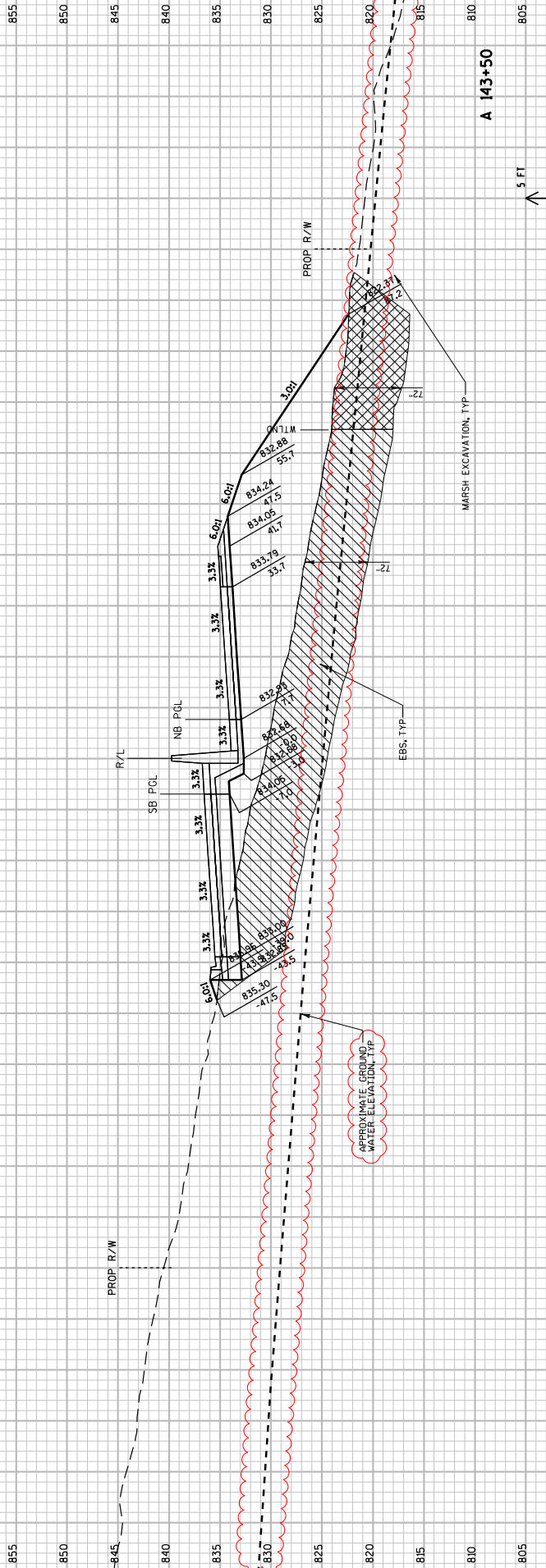
Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1526  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A



Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1527  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A

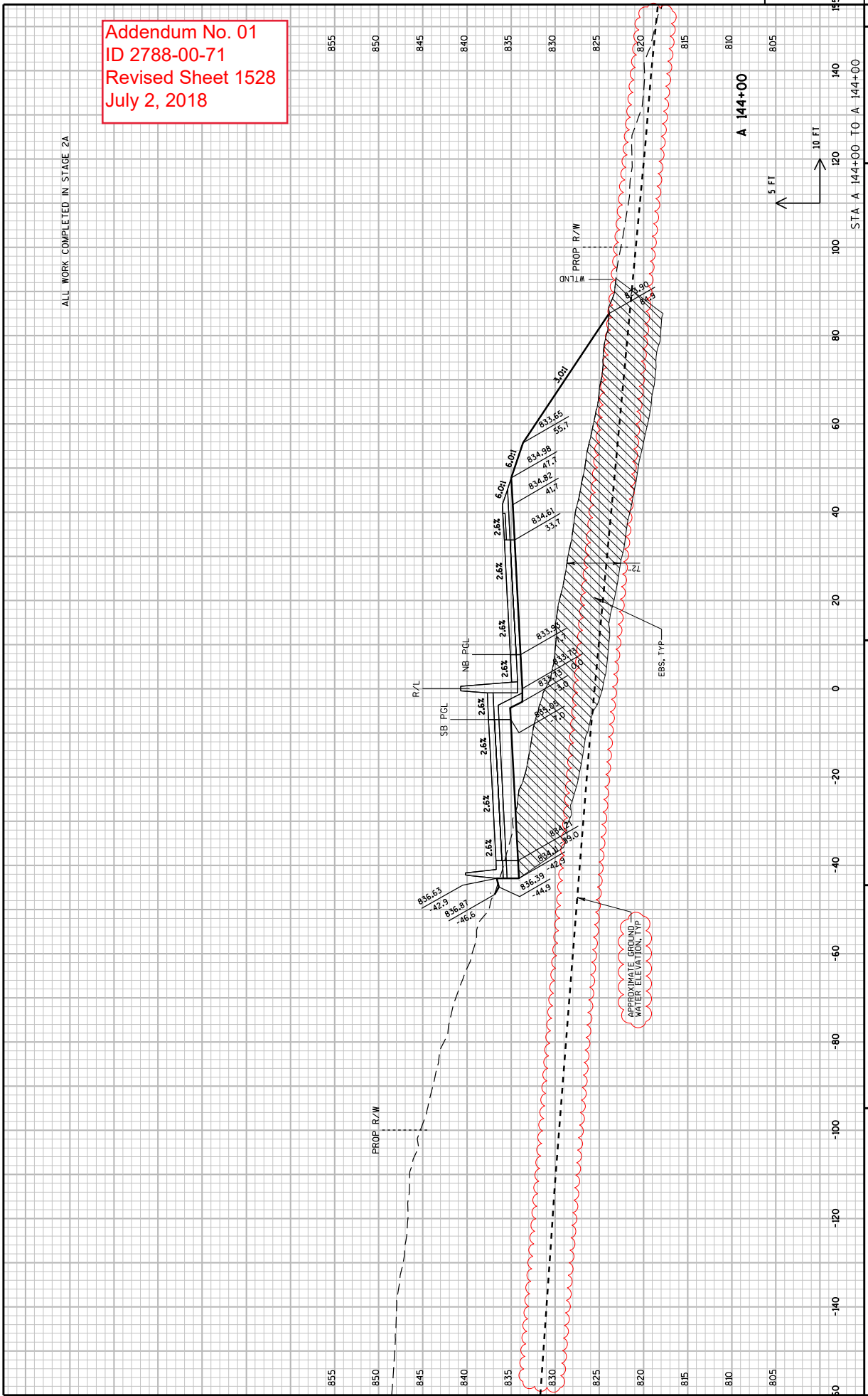


PROJECT NO: 2788-00-71  
 COUNTY: WAUKESHA  
 CROSS SECTIONS: WAUKESHA BYPASS  
 SHEET 1527

FILE NAME : \\wplw00\lmgpr\proj\59971\1\cads\dy\_090201\_xs\_wb.dgn  
 PLOT DATE : 6/26/2018 10:25:55 AM  
 PLOT BY : fhw.lpw@tm  
 PLOT SCALE : 20:1  
 WISDOT/CADS SHEET 21

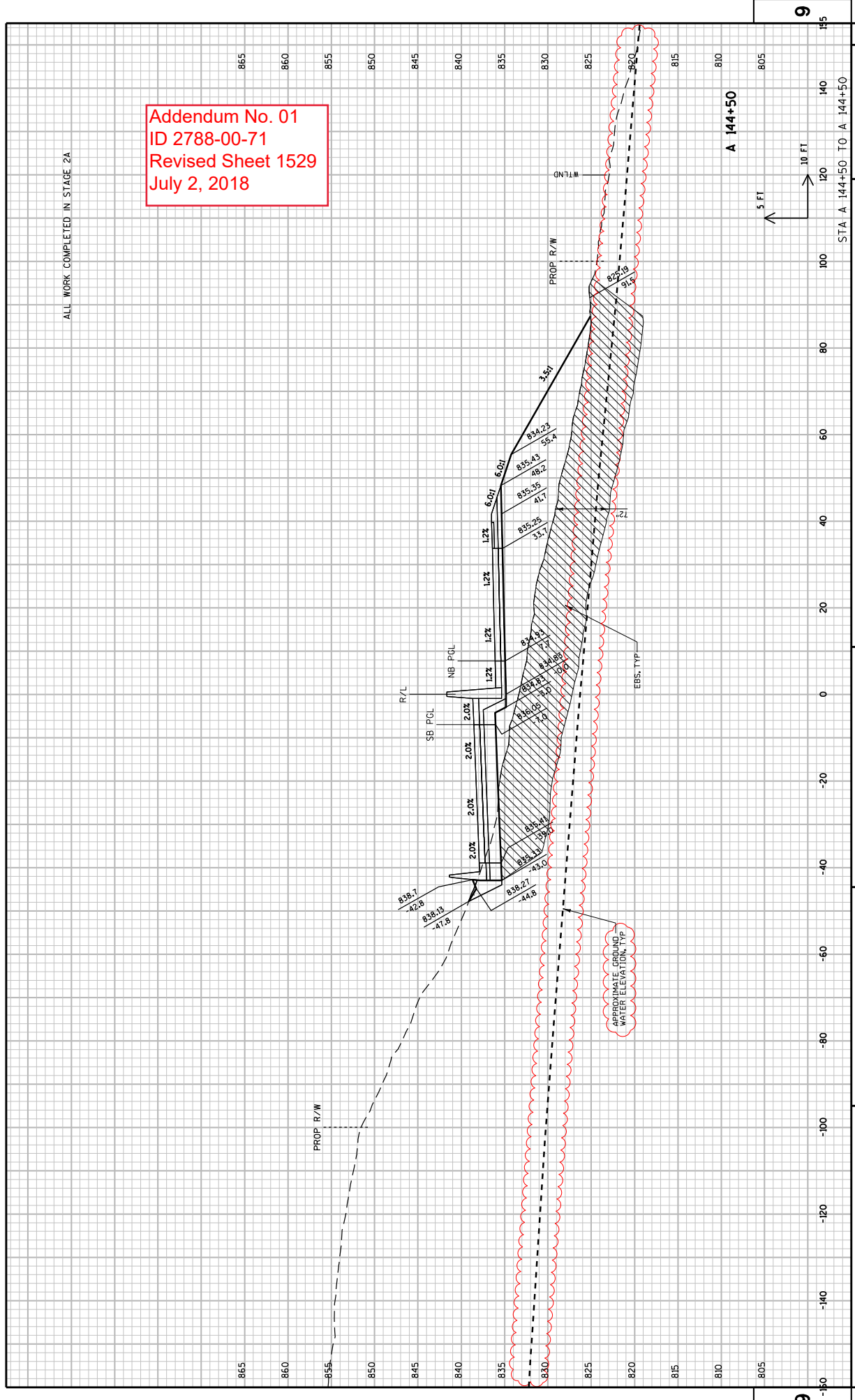
Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1528  
July 2, 2018

ALL WORK COMPLETED IN STAGE 2A



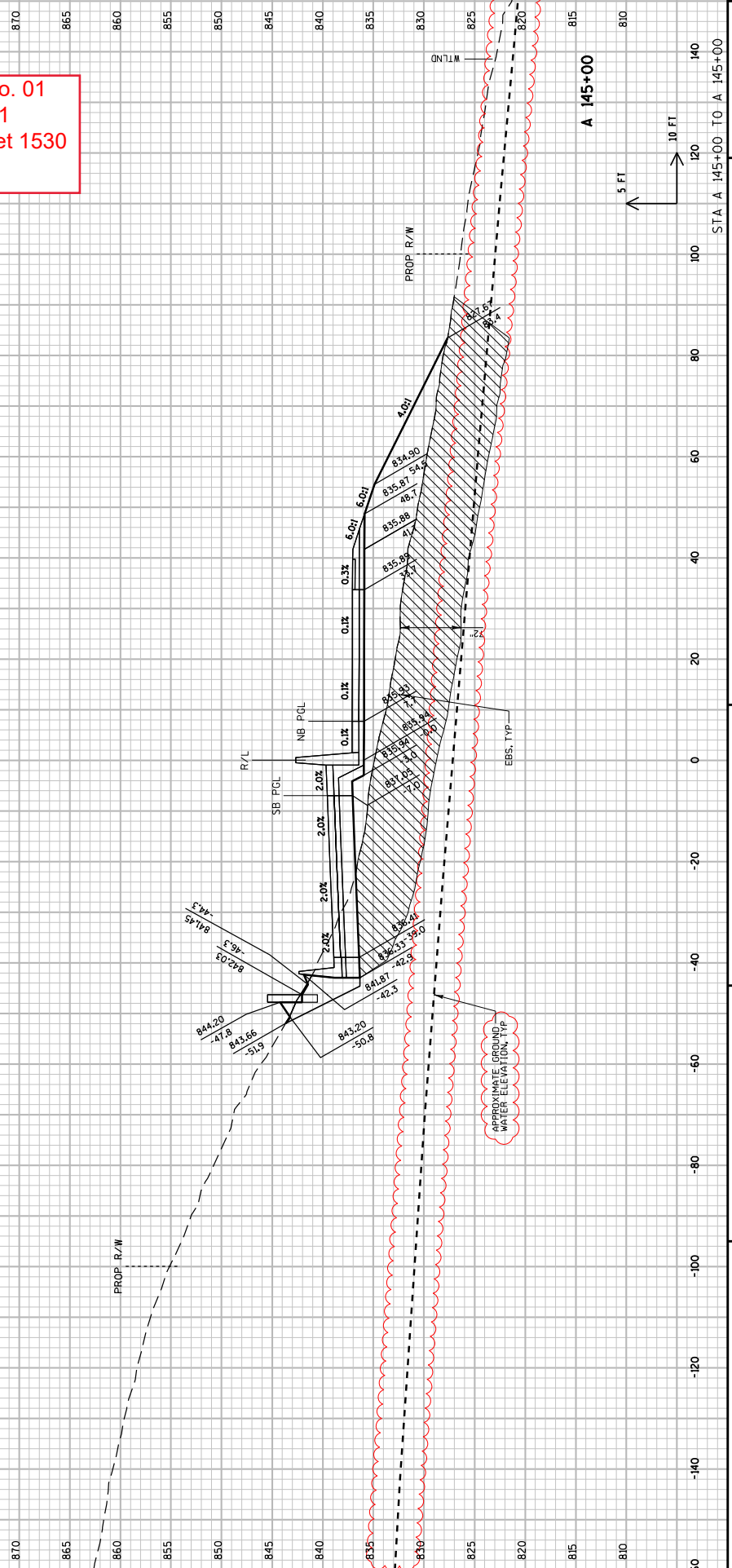
ALL WORK COMPLETED IN STAGE 2A

Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1529  
July 2, 2018



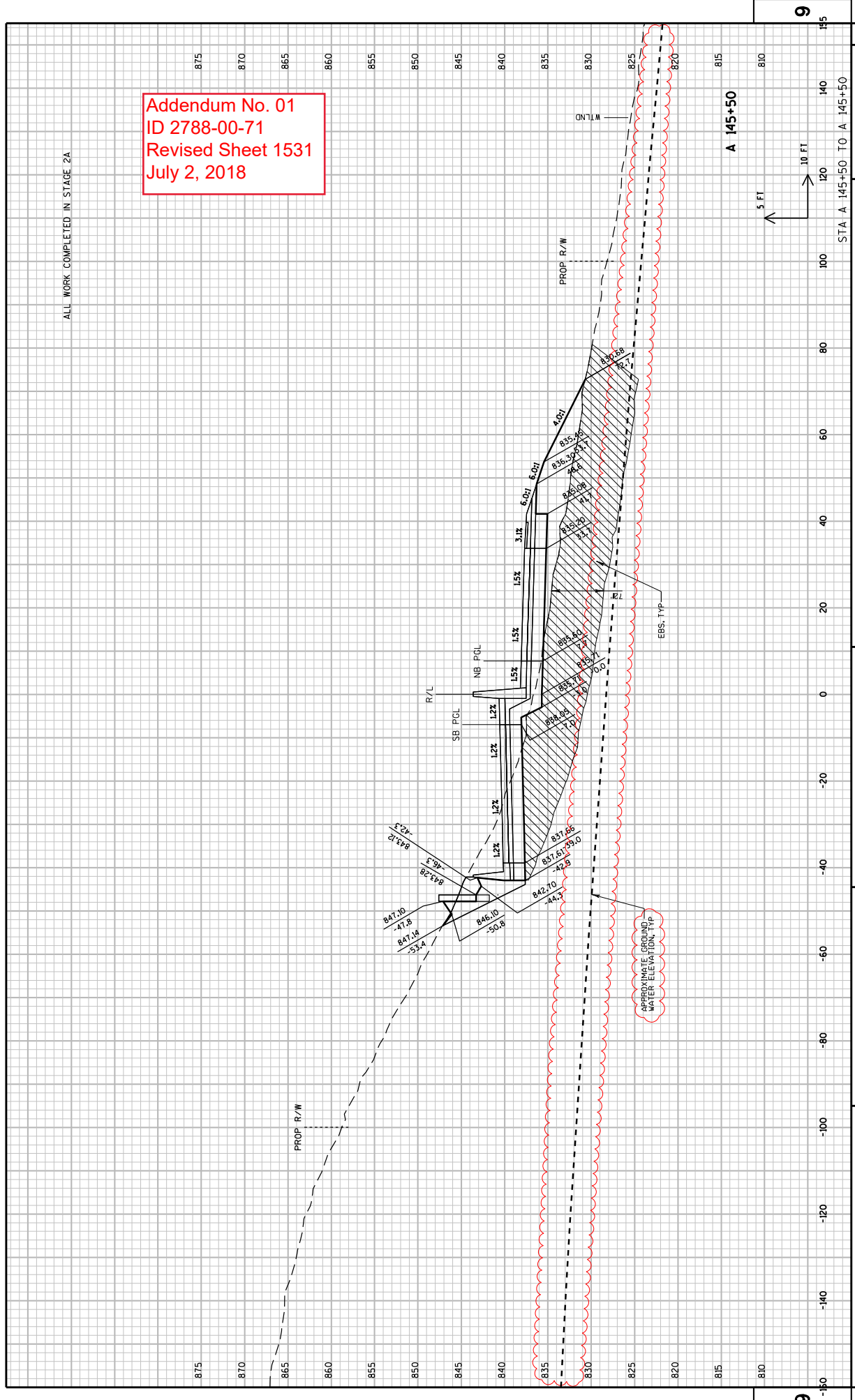
ALL WORK COMPLETED IN STAGE 2A

Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1530  
July 2, 2018



ALL WORK COMPLETED IN STAGE 2A

Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1531  
July 2, 2018



PROJECT NO: 2788-00-71

HWY: WAUKESHA BYPASS

COUNTY: WAUKESHA

CROSS SECTIONS: WAUKESHA BYPASS

SHEET 1531

STA A 145+50 TO A 145+50

9

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FILE NAME : \\nw11w00\jngpr\proj\59971\1\cadd\by\_090201\_xs\_wb.dgn

PLOT DATE : 6/26/2018 10:26:01 AM

PLOT BY : fhw11p@lin

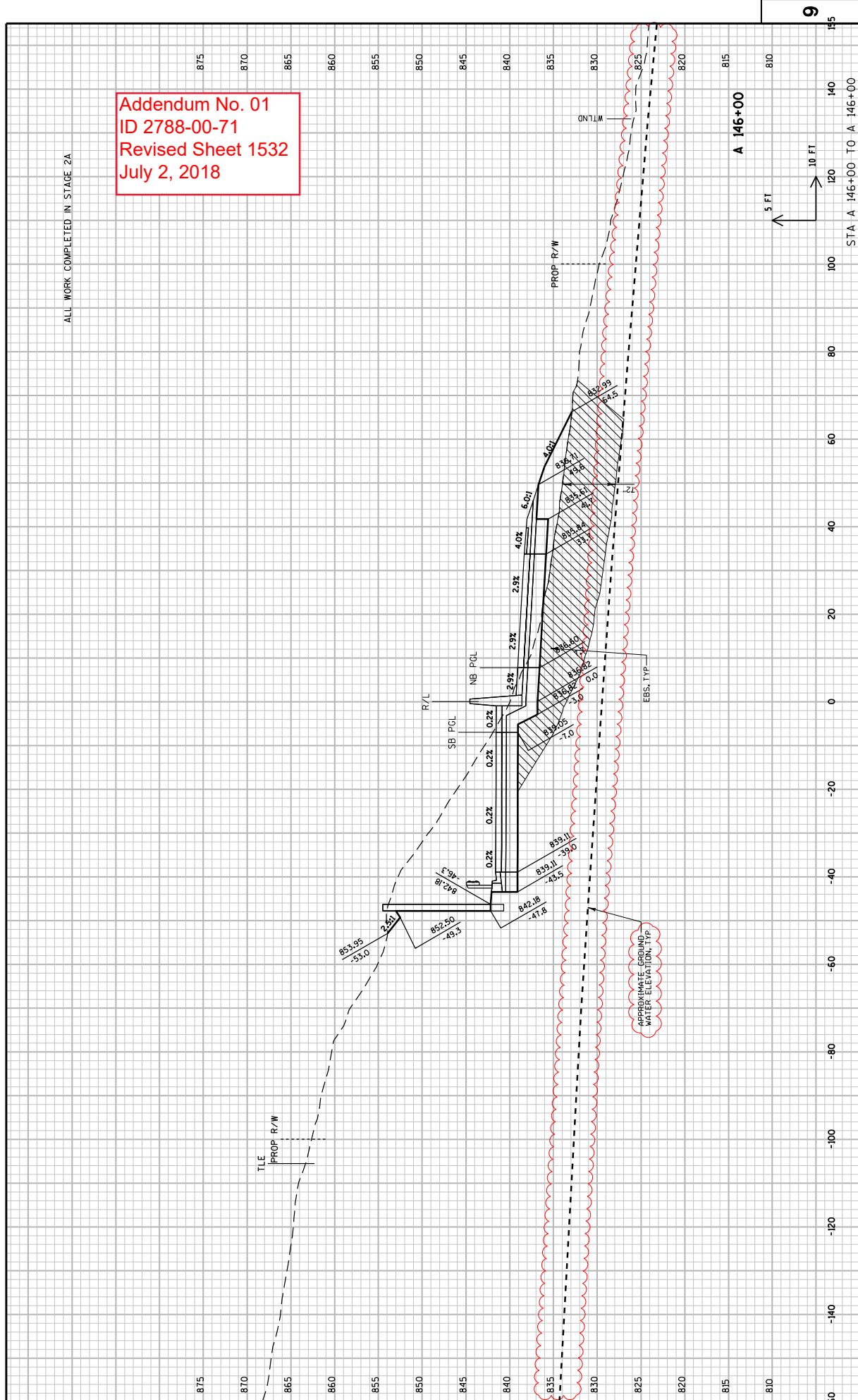
PLOT SCALE : 20:1

WISDOT/CADD SHEET 21



ALL WORK COMPLETED IN STAGE 2A

Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1532  
July 2, 2018

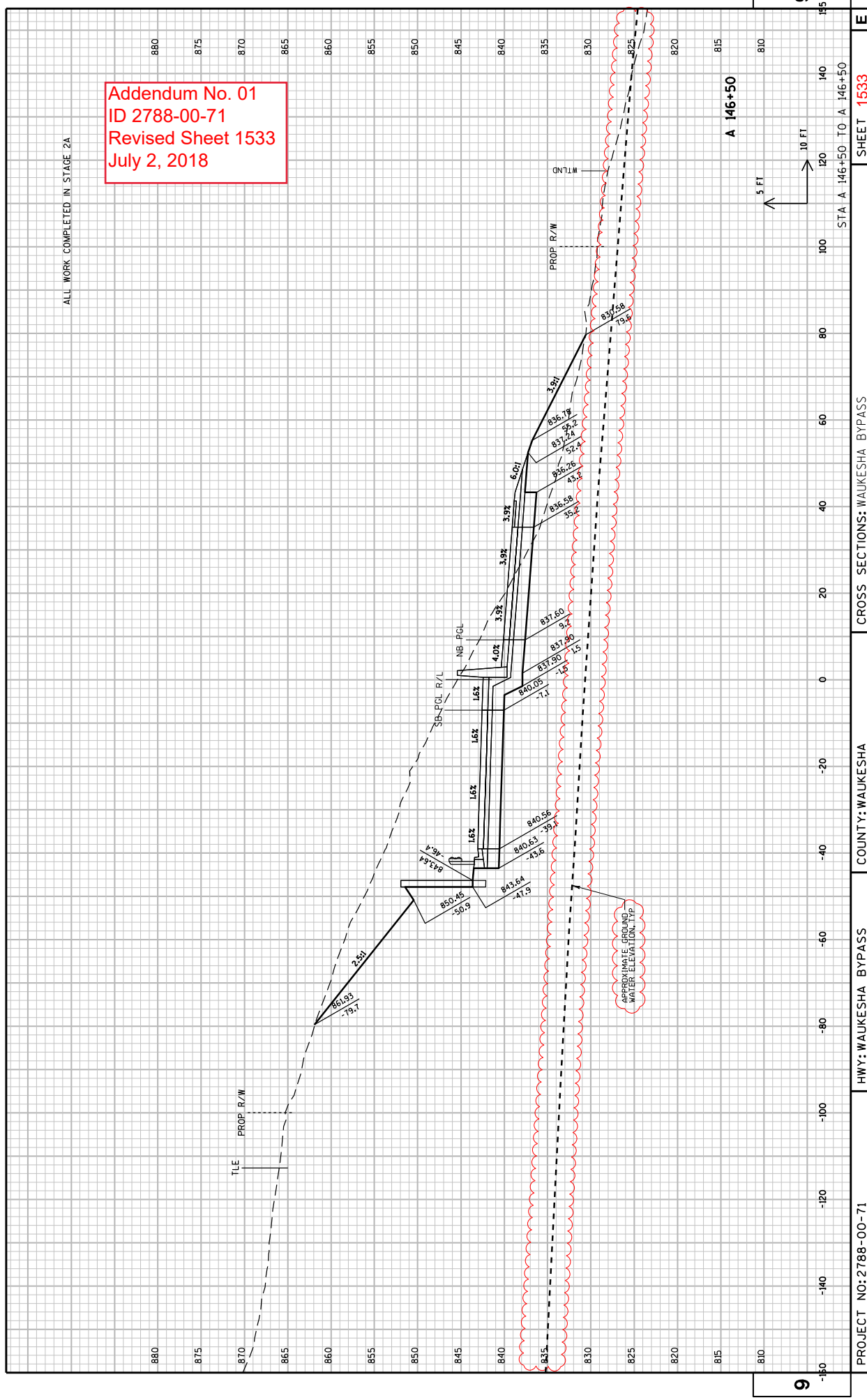


PROJECT NO: 2788-00-71  
 COUNTY: WAUKESHA  
 CROSS SECTIONS: WAUKESHA BYPASS  
 SHEET 1532  
 E

FILE NAME : \\nw11w00\jngpr\proj\59971\1\cadd\by\_090201\_xs\_wb.dgn  
 PLOT DATE : 6/26/2018 10:26:02 AM  
 PLOT BY : TFW.LP@ihr  
 PLOT SCALE : 20:1  
 WISDOT/CADD SHEET 21

Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1533  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A



5 FT  
 10 FT

A 146+50

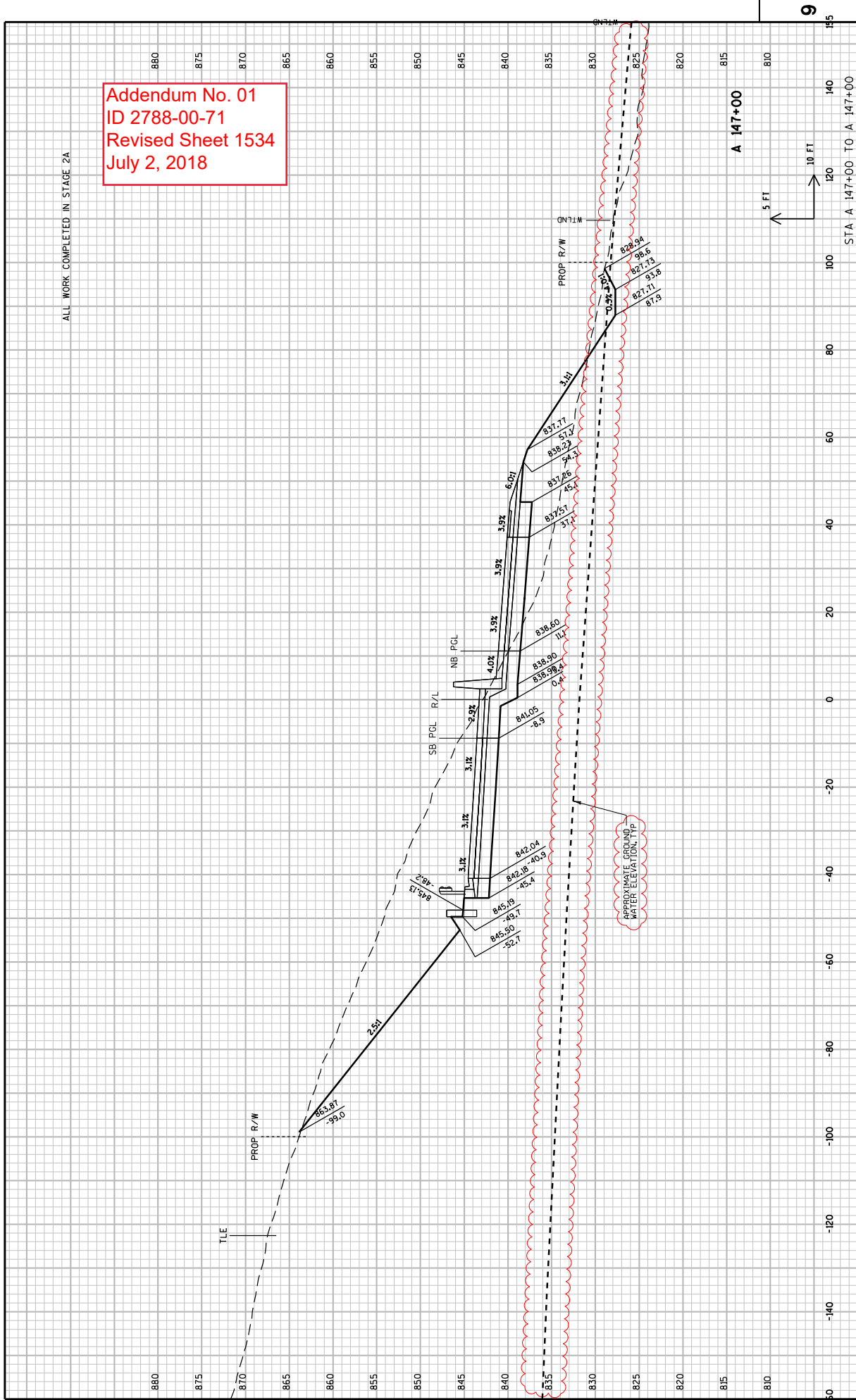
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PROJECT NO: 2788-00-71 COUNTY: WAUKESHA CROSS SECTIONS: WAUKESHA BYPASS SHEET 1533 E  
 FILE NAME : \\wplw001\proj\proj\59971\1\acis\by\_090201\_XS\_WB.dgn PLOT BY : TWW\JPB\JM PLOT DATE : 6/26/2018 10:26:03 AM PLOT SCALE : 20:1 WISDOT/CADS SHEET 21

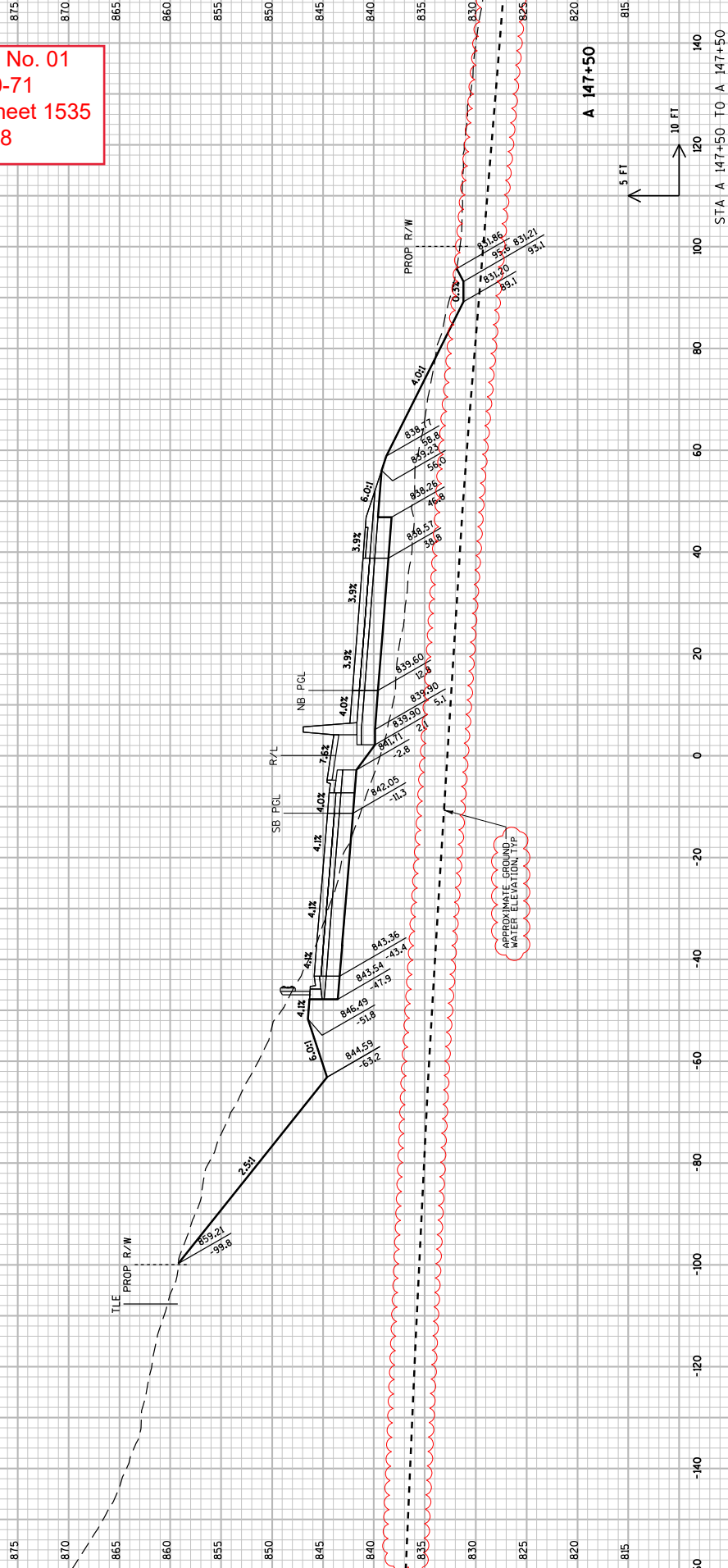
Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1534  
July 2, 2018

ALL WORK COMPLETED IN STAGE 2A



ALL WORK COMPLETED IN STAGE 2A

Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1535  
July 2, 2018

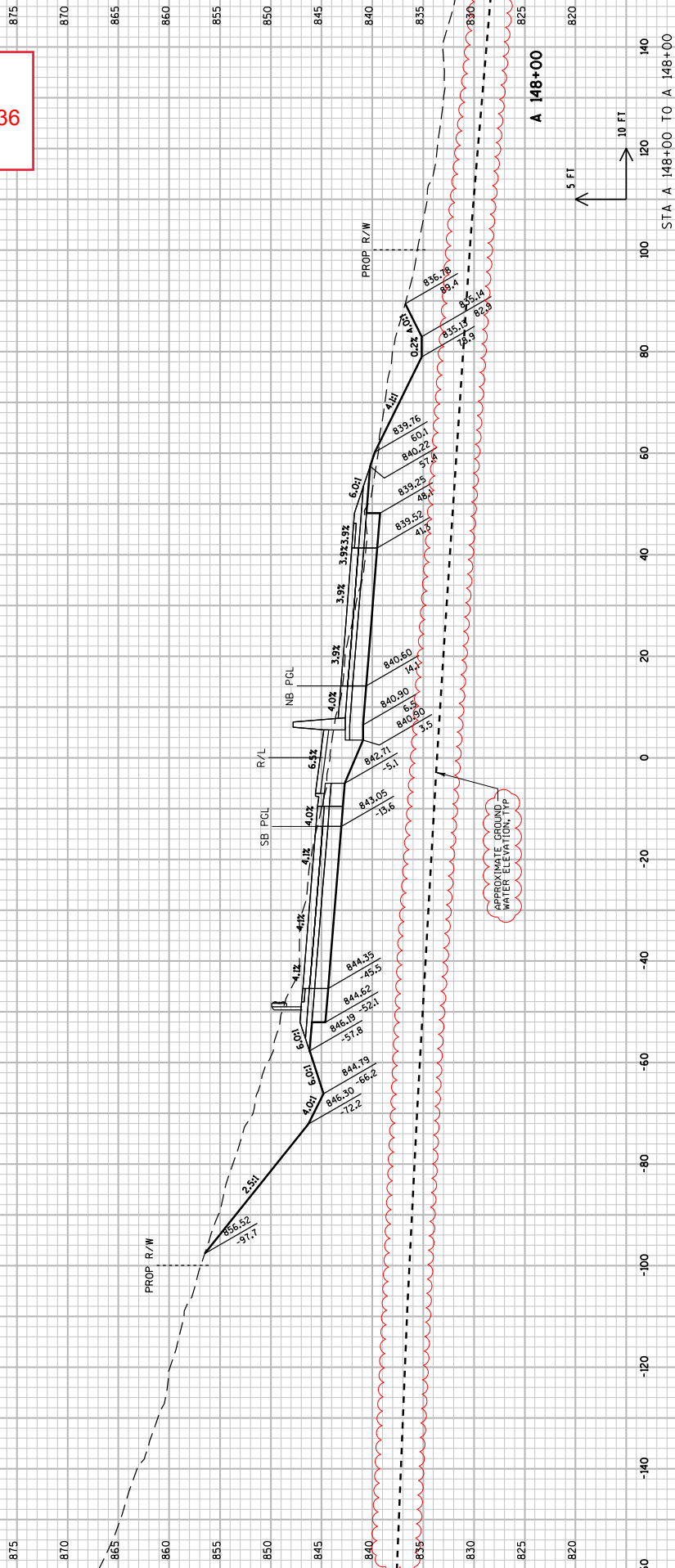


PROJECT NO: 2788-00-71  
 COUNTY: WAUKESHA  
 CROSS SECTIONS: WAUKESHA BYPASS  
 SHEET 1535  
 E

FILE NAME : \\nw11w00\jngpr\proj\59971\1\cads\wy\_090201\_xs\_wb.dgn  
 PLOT DATE : 6/26/2018 10:26:04 AM  
 PLOT BY : TFW.LP@JRM  
 PLOT SCALE : 20:1  
 WISDOT/CADD SHEET 21

ALL WORK COMPLETED IN STAGE 2A

Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1536  
July 2, 2018



5 FT

10 FT

10 FT

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PROJECT NO: 2788-00-71 COUNTY: WAUKESHA HWY: WAUKESHA BYPASS CROSS SECTIONS: WAUKESHA BYPASS SHEET 1536

FILE NAME : \\wplw001\proj\proj\59971\1\cadd\by\_090201\_xs\_mb.dgn PLOT DATE : 6/26/2018 10:26:05 AM PLOT BY : fhw.lhp@im PLOT SCALE : 20:1 WISDOT/CADD SHEET 21

ALL WORK COMPLETED IN STAGE 2A

Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1537  
July 2, 2018

875

870

865

860

855

850

845

840

835

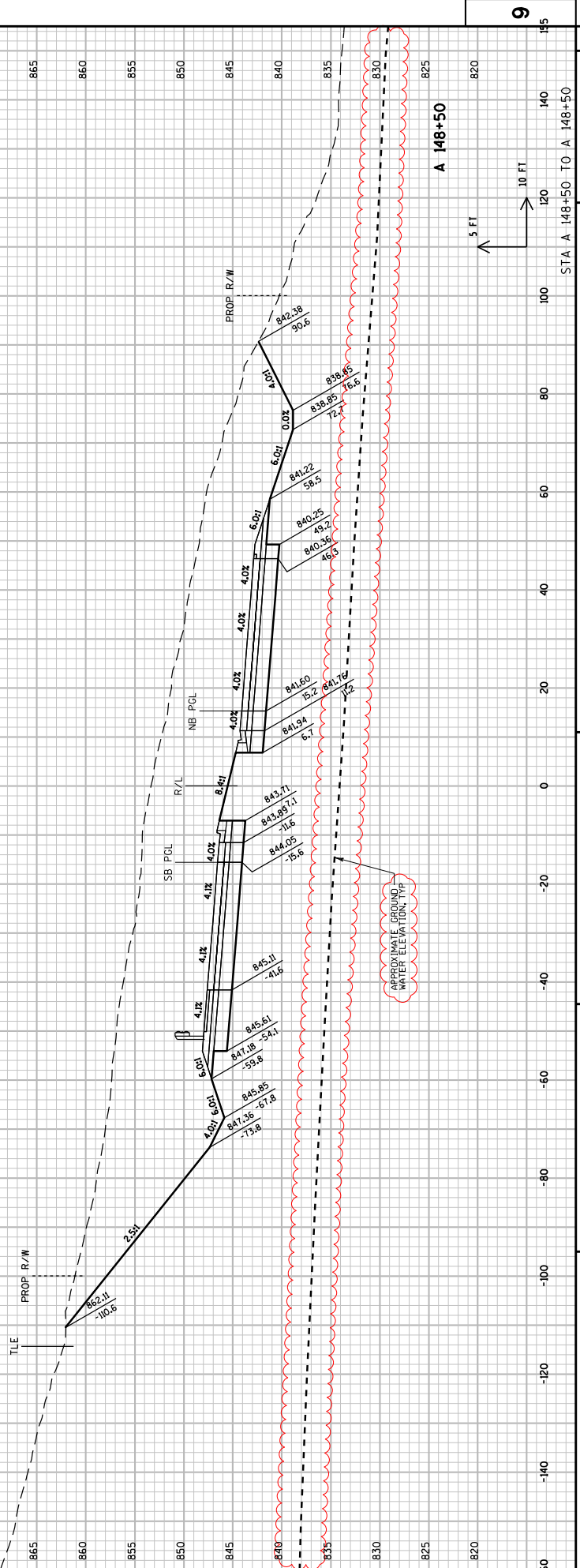
830

825

820

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5 FT

10 FT

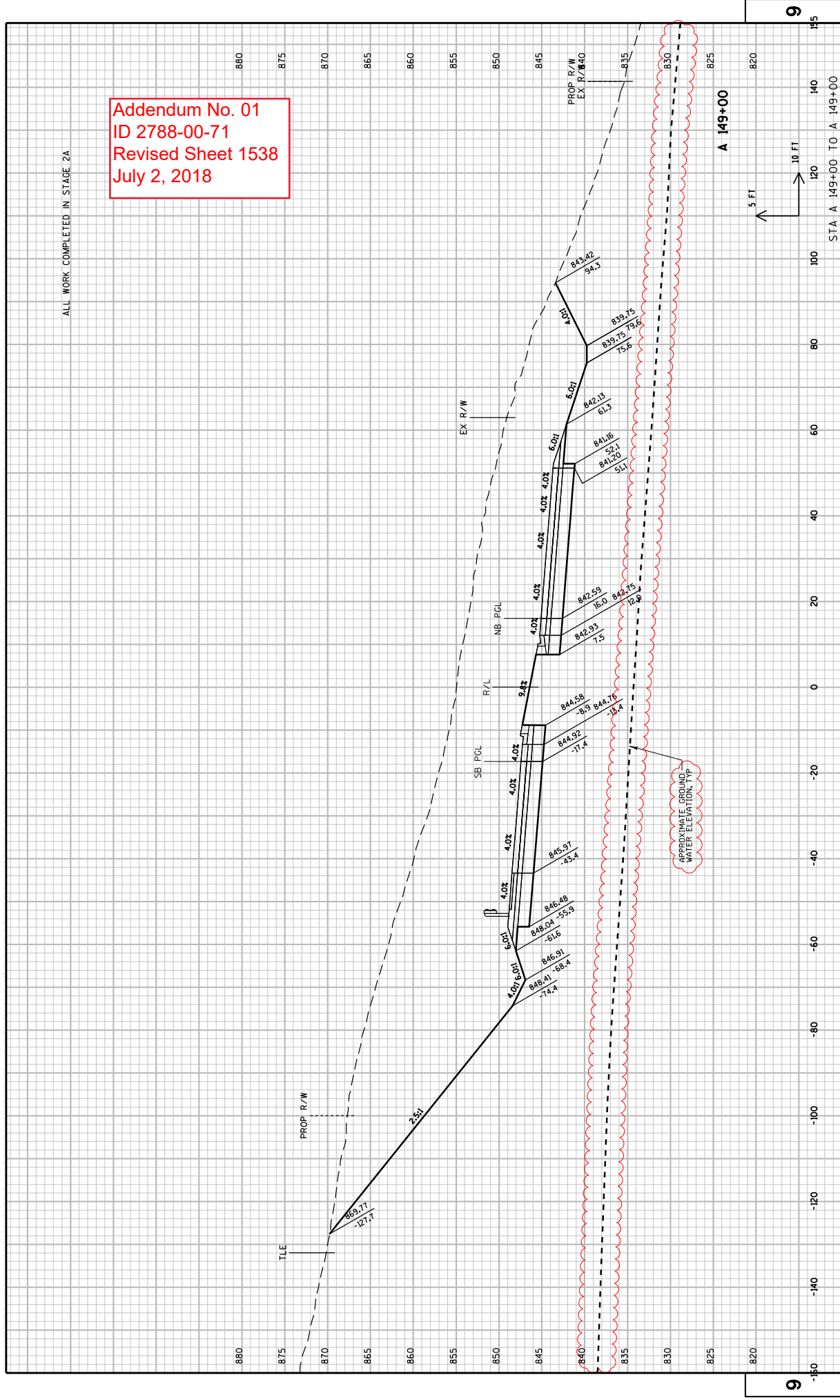
A 148+50

PROJECT NO: 2788-00-71  
COUNTY: WAUKESHA  
HWY: WAUKESHA BYPASS  
CROSS SECTIONS: WAUKESHA BYPASS  
SHEET 1537  
STA A 148+50 TO A 148+50

FILE NAME : \\nw11w00\lmgpr\proj\59971\1\cadd\by\_090201.XS.WB.dgn  
PLOT DATE : 6/26/2018 10:26:06 AM  
PLOT BY : TFW.LP@JLR  
PLOT SCALE : 20:1  
WISDOT/CADD SHEET 21

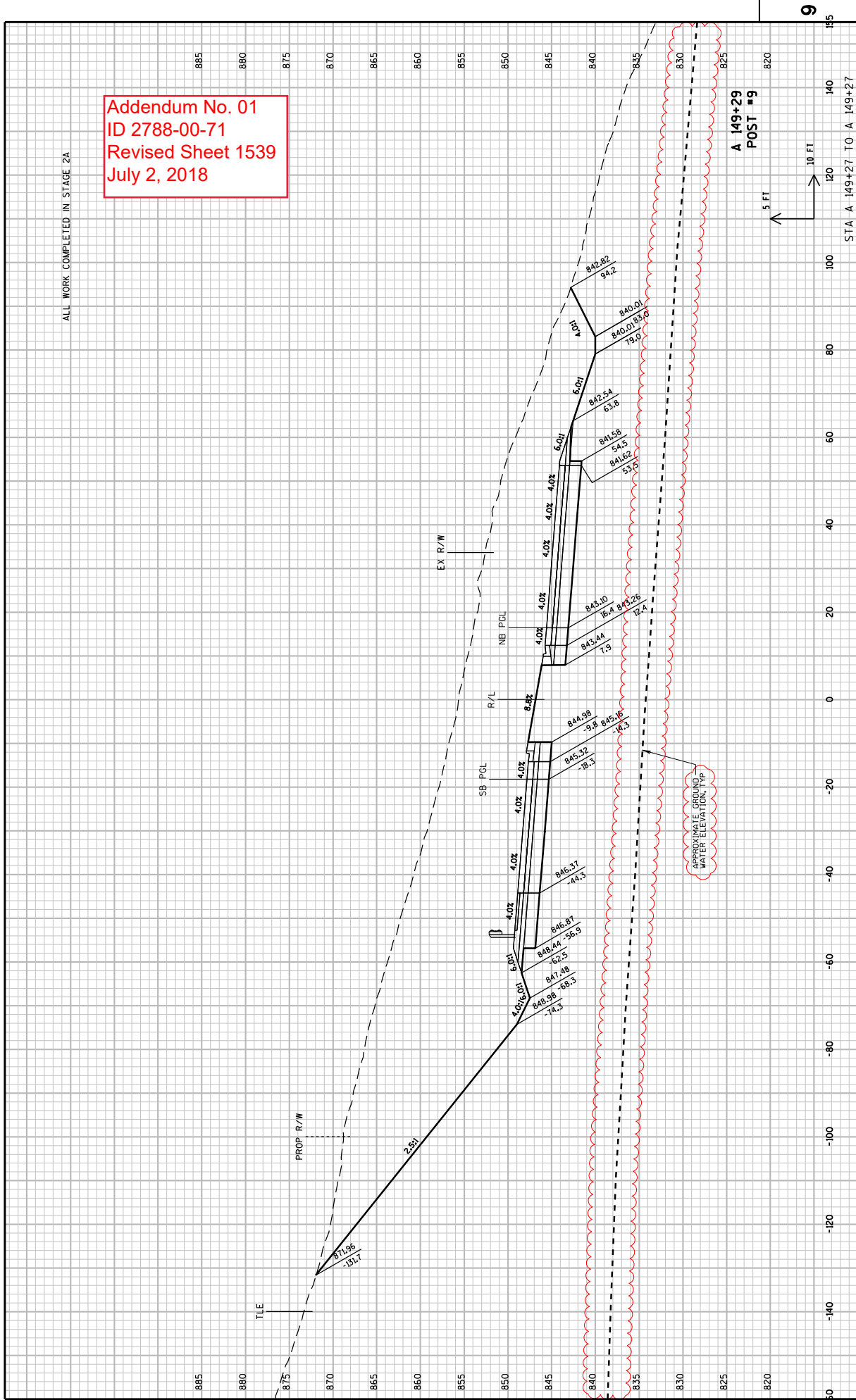
ALL WORK COMPLETED IN STAGE 2A

Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1538  
July 2, 2018



Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1539  
 July 2, 2018

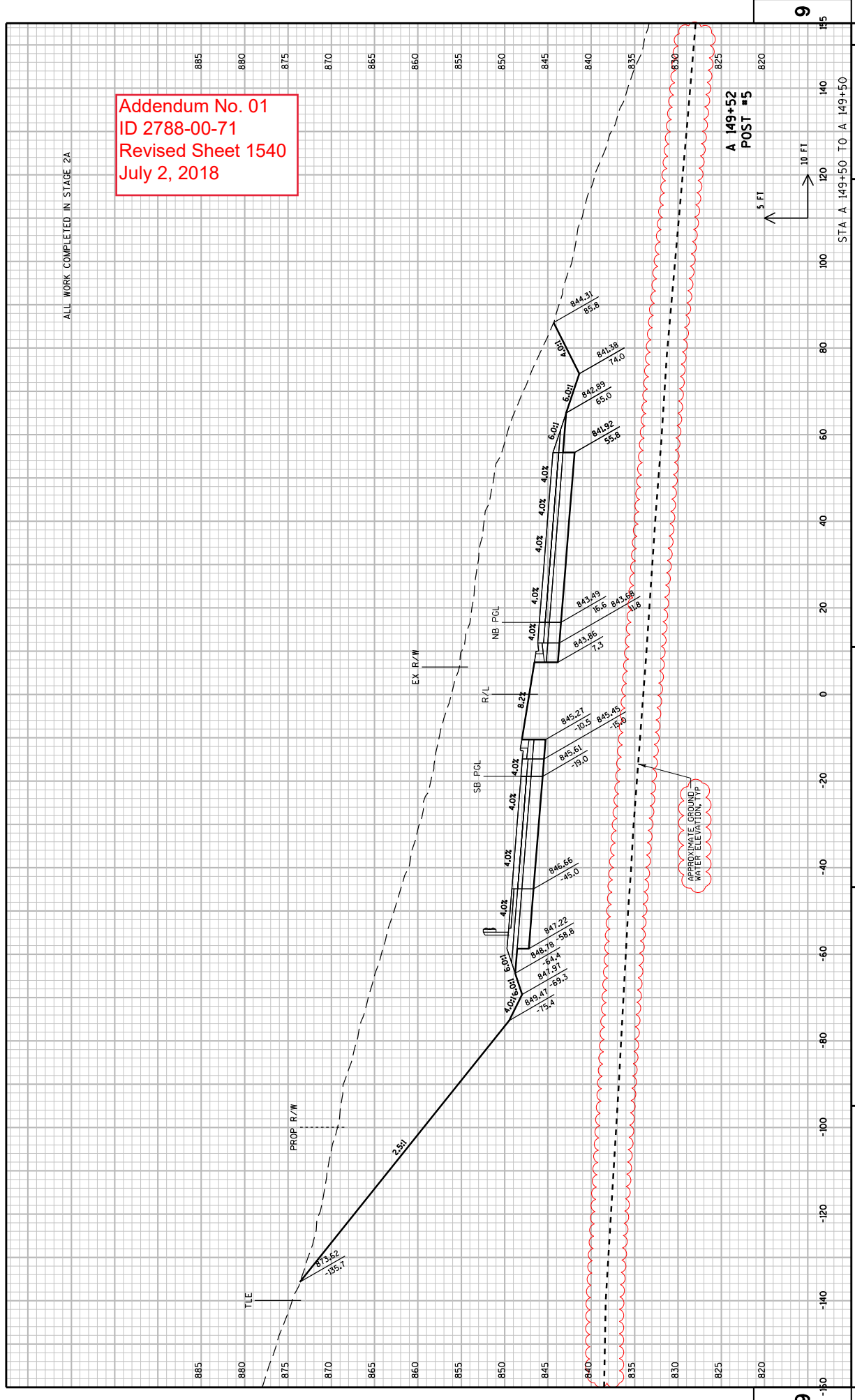
ALL WORK COMPLETED IN STAGE 2A





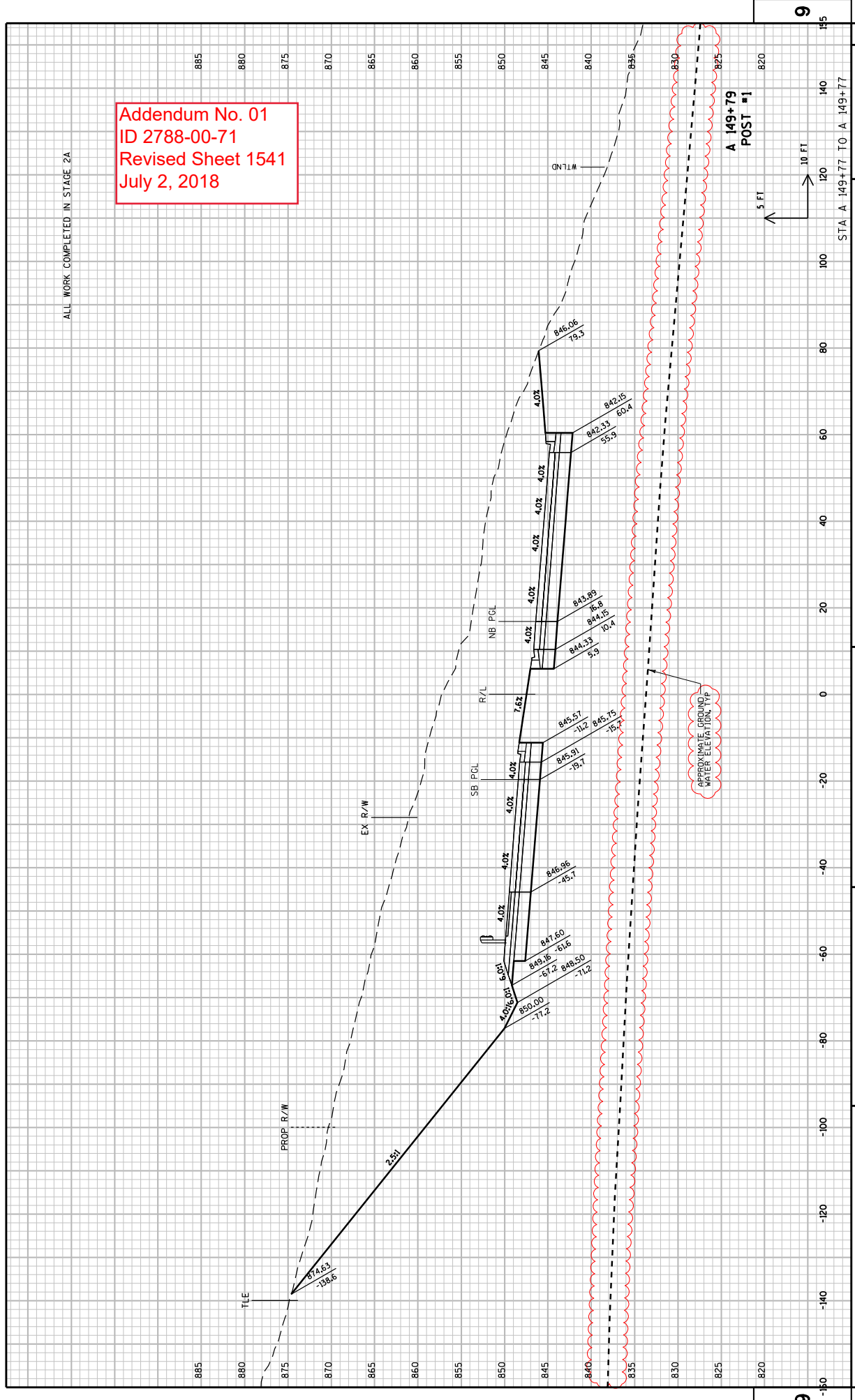
ALL WORK COMPLETED IN STAGE 2A

Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1540  
July 2, 2018



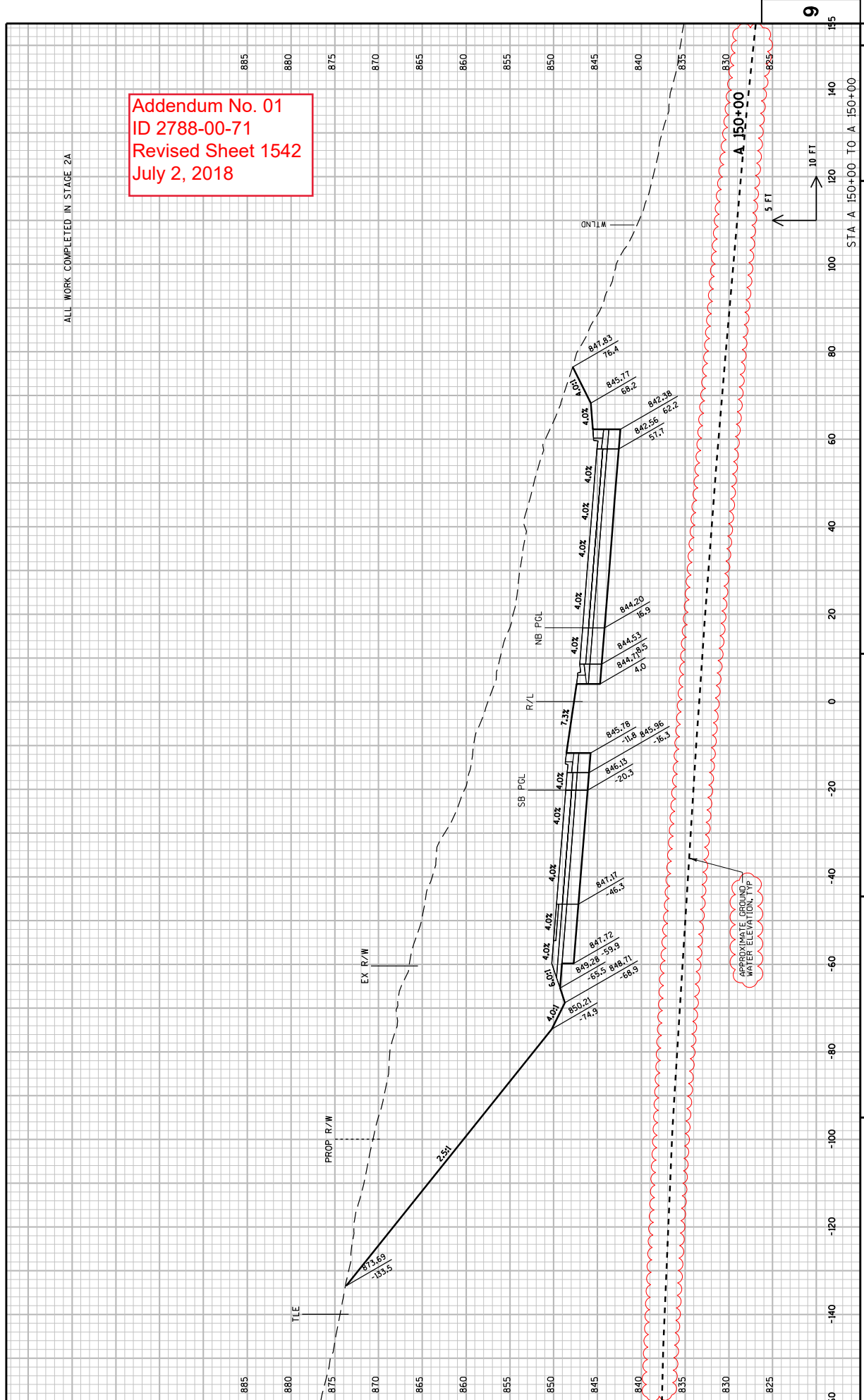
ALL WORK COMPLETED IN STAGE 2A

Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1541  
July 2, 2018



ALL WORK COMPLETED IN STAGE 2A

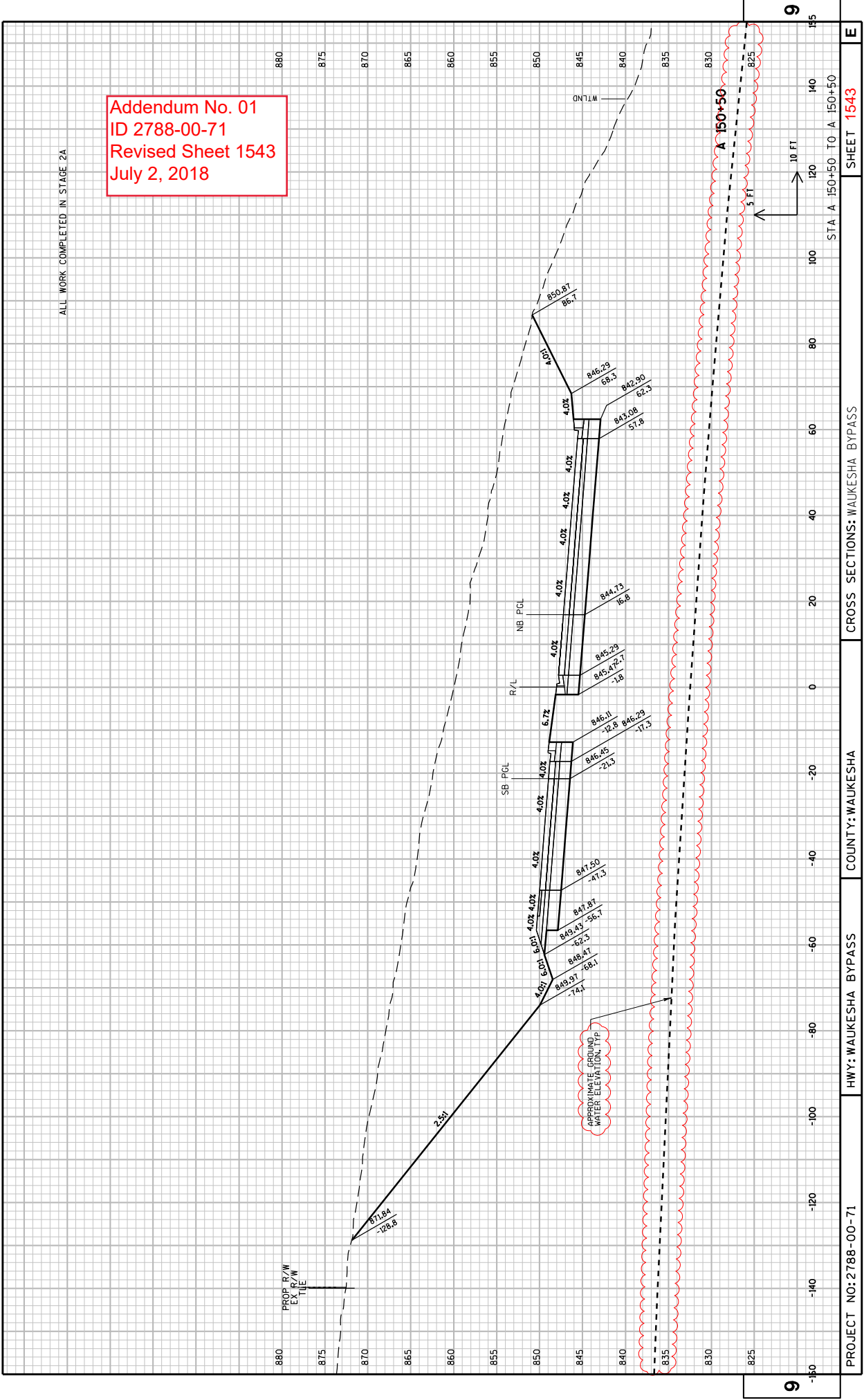
Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1542  
July 2, 2018



|                                                                 |                  |                                   |            |
|-----------------------------------------------------------------|------------------|-----------------------------------|------------|
| PROJECT NO: 2788-00-71                                          | COUNTY: WAUKESHA | CROSS SECTIONS: WAUKESHA BYPASS   | SHEET 1542 |
| HWY: WAUKESHA BYPASS                                            |                  | PLOT NAME :                       |            |
| FILE NAME : \\nw11w00\lmp\proj\59971\1\cads\by_090201_xs_wb.dgn |                  | PLOT DATE : 6/26/2018 10:26:10 AM |            |
|                                                                 |                  | PLOT BY : TML\PLM\JM              |            |
|                                                                 |                  | PLOT SCALE : 20:1                 |            |
|                                                                 |                  | WISDOT/CADD SHEET 21              |            |

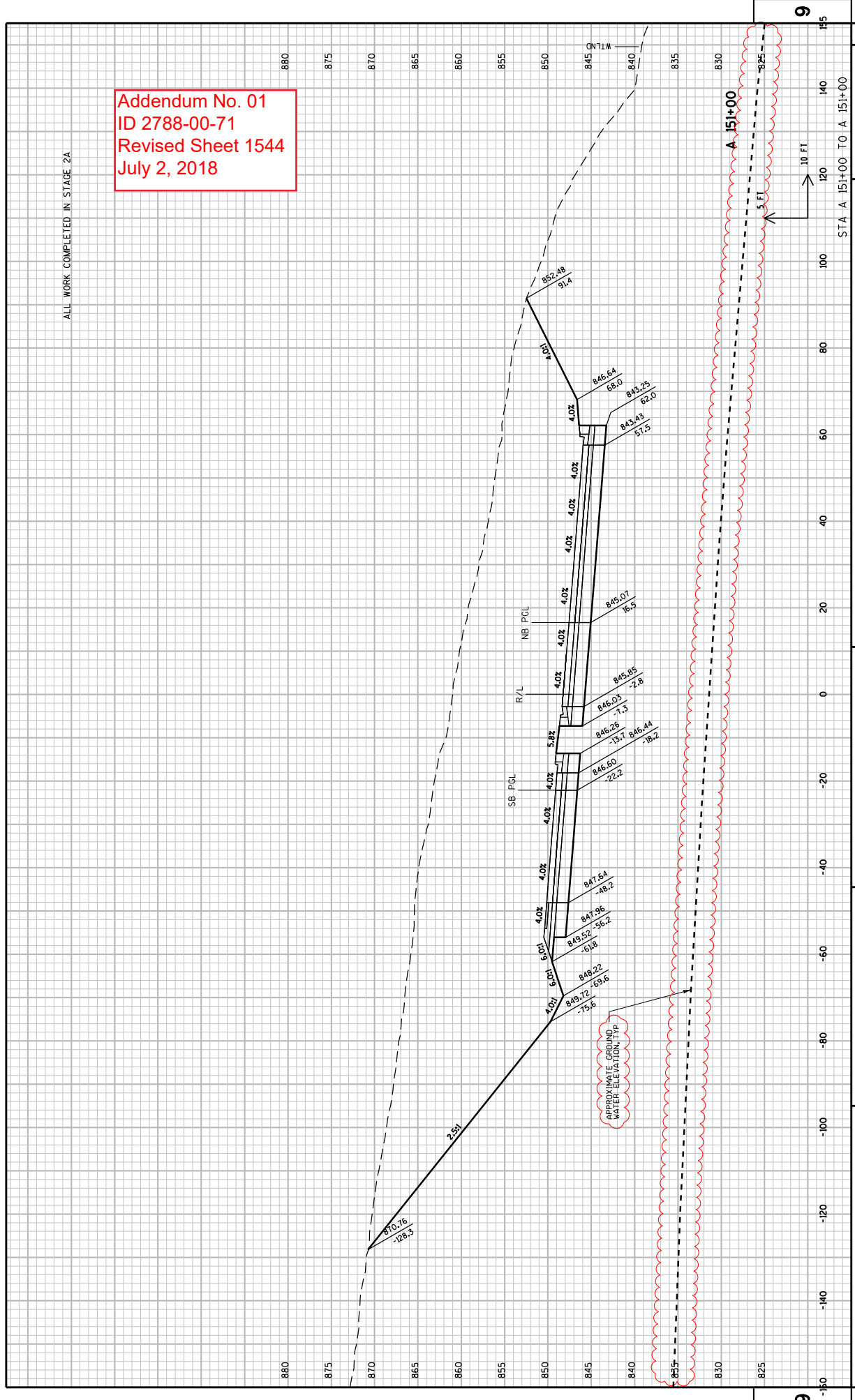
ALL WORK COMPLETED IN STAGE 2A

Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1543  
 July 2, 2018



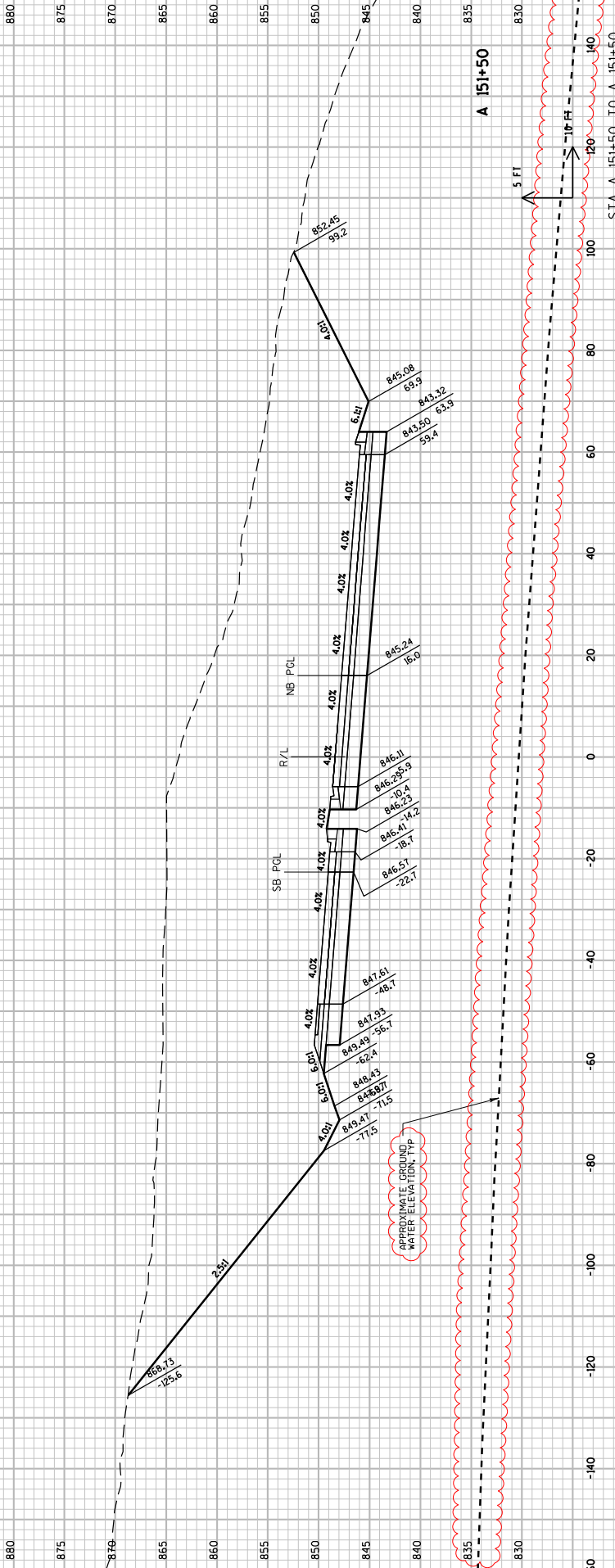
ALL WORK COMPLETED IN STAGE 2A

Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1544  
July 2, 2018



Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1545  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A

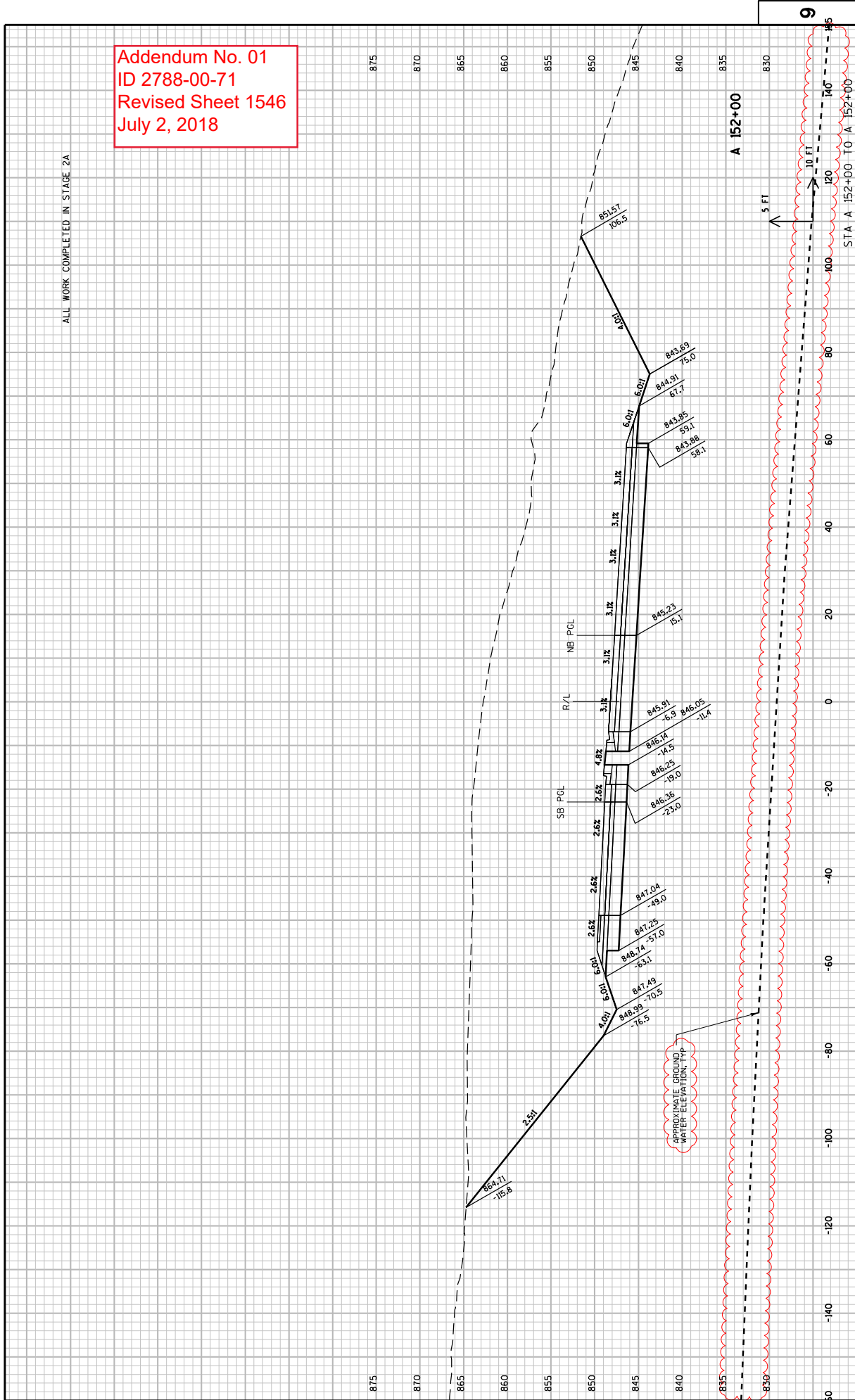


PROJECT NO: 2788-00-71 COUNTY: WAUKESHA CROSS SECTIONS: WAUKESHA BYPASS SHEET 1545

FILE NAME : \\wplw00\lmgpr\proj\59971\1\cadd\by\_090201\_xs\_mb.dgn PLOT DATE : 6/26/2018 10:26:14 AM PLOT BY : fhw.lpw@hr PLOT SCALE : 20:1 WISDOT/CADD SHEET 21

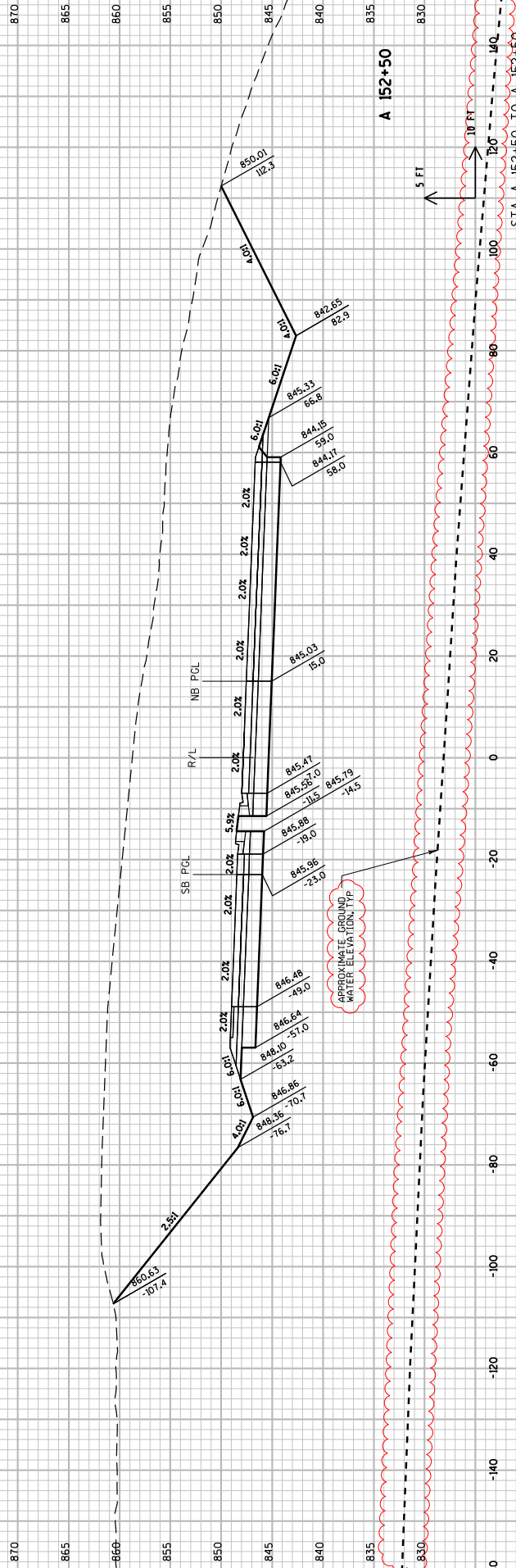
Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1546  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A



Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1547  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A

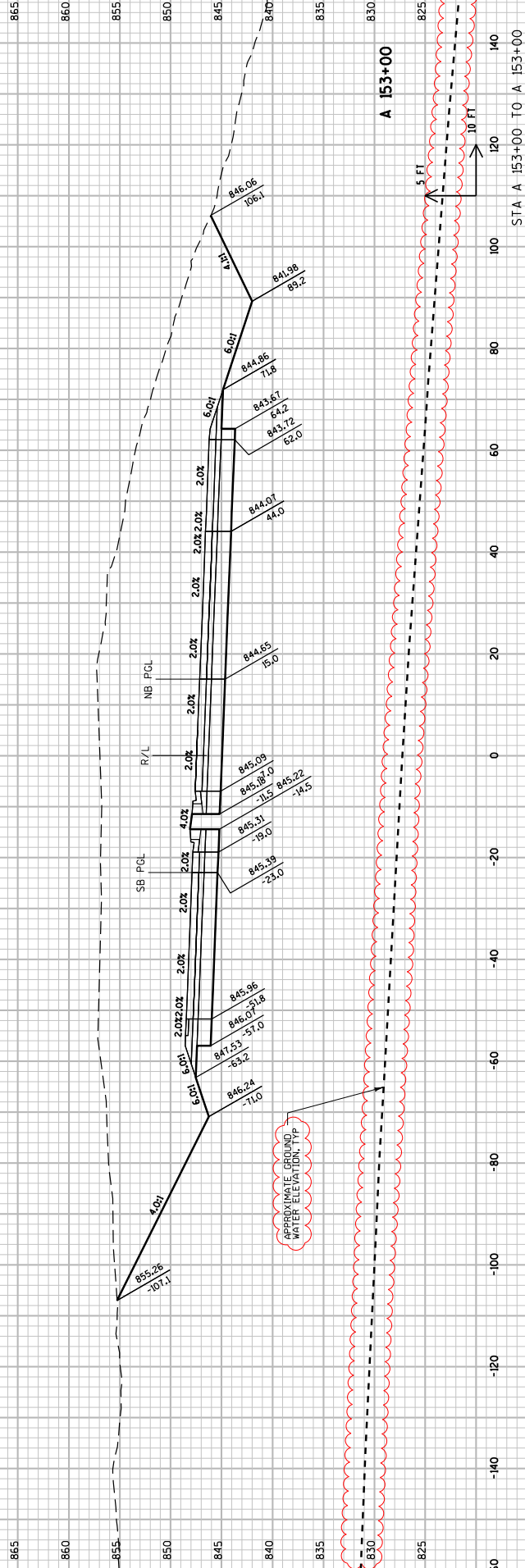


PROJECT NO: 2788-00-71  
 COUNTY: WAUKESHA  
 CROSS SECTIONS: WAUKESHA BYPASS  
 SHEET 1547



ALL WORK COMPLETED IN STAGE 2B

Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1548  
July 2, 2018

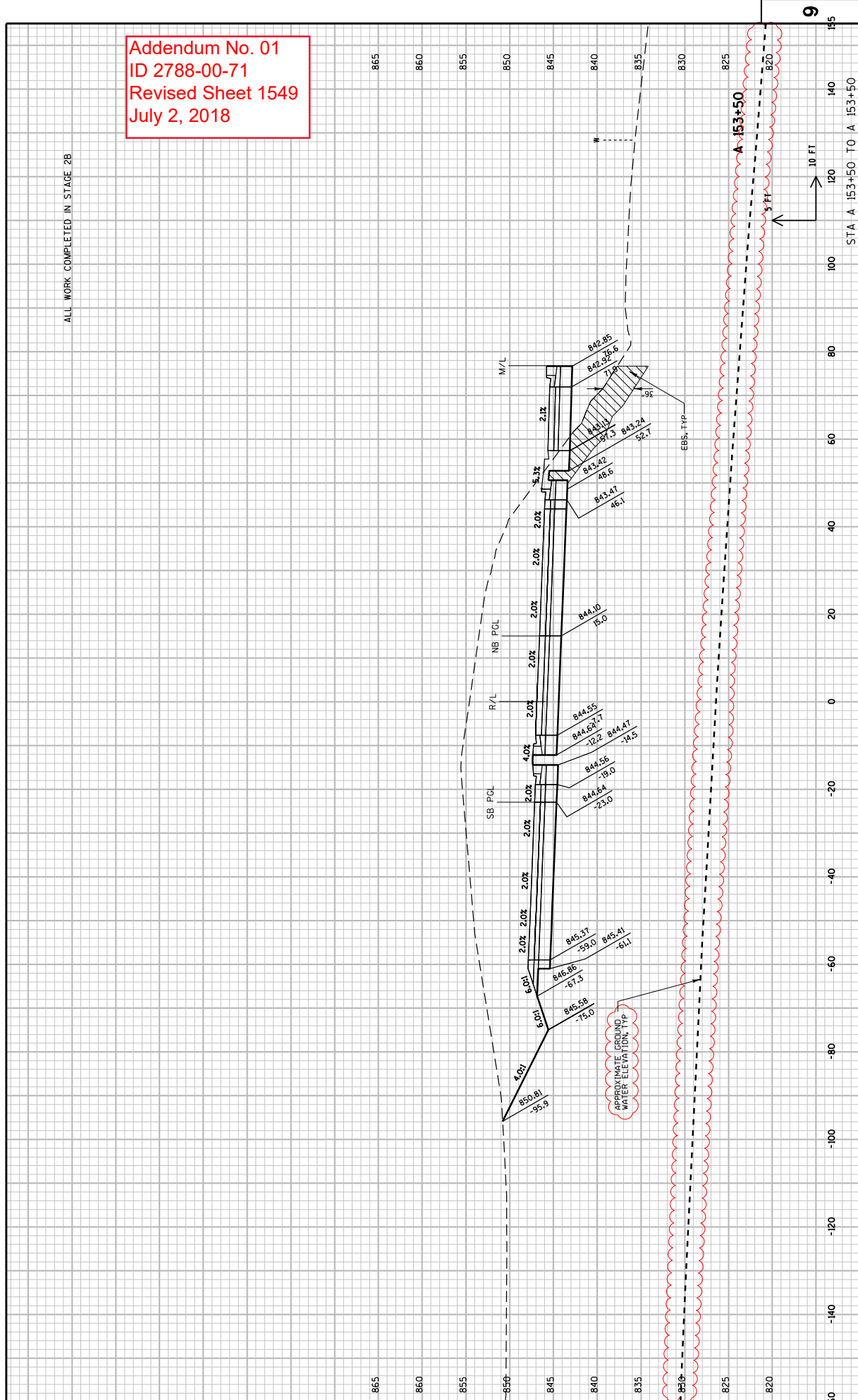


PROJECT NO: 2788-00-71  
 COUNTY: WAUKESHA  
 HWY: WAUKESHA BYPASS  
 CROSS SECTIONS: WAUKESHA BYPASS  
 SHEET 1548  
 STA A 153+00 TO A 153+00

FILE NAME : \\wplw00\lmp\proj\59971\1\cadd\by\_090201\_xs\_wb.dgn  
 PLOT BY : TFW.LP@WI  
 PLOT DATE : 6/26/2018 10:26:17 AM  
 PLOT SCALE : 20:1  
 WISDOT/CADD SHEET 21

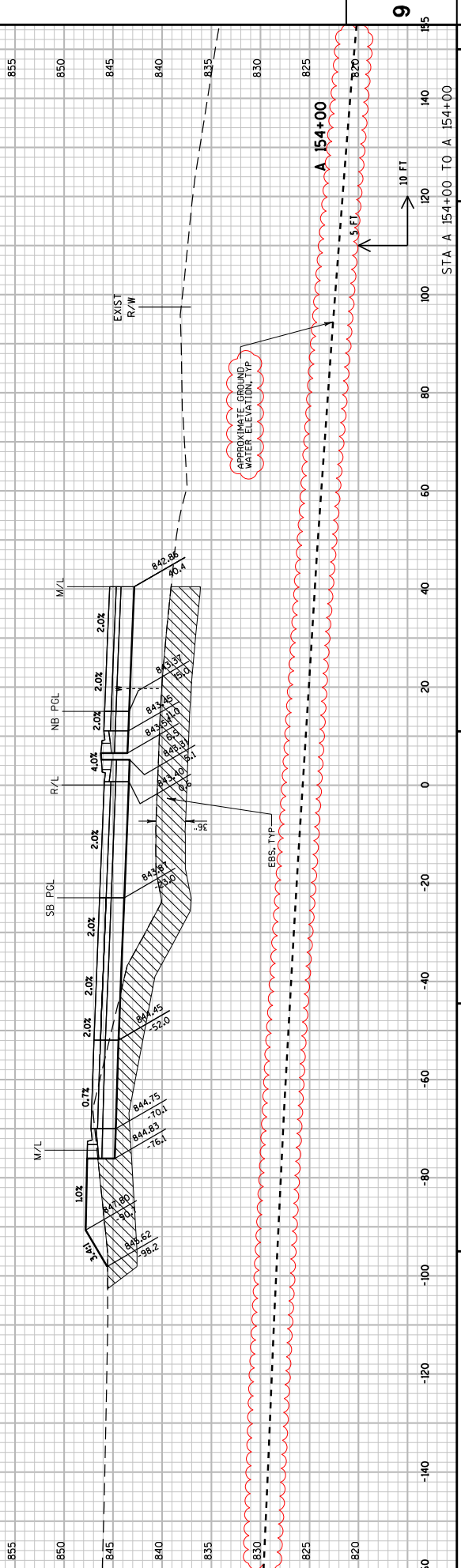
Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1549  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2B



Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1550  
July 2, 2018

ALL WORK COMPLETED IN STAGE 2B



PROJECT NO: 2788-00-71

CROSS SECTIONS: WAUKESHA BYPASS

COUNTY: WAUKESHA

HWY: WAUKESHA BYPASS

SHEET 1550

FILE NAME : \\nw11w00\l\proj\proj\59971\1\cadd\by\_090201\_xs\_mb.dgn

PLOT DATE : 6/26/2018 10:26:19 AM

PLOT BY : twl1p1m1m

CROSS SECTIONS: WAUKESHA BYPASS

PLOT NAME :

COUNTY: WAUKESHA

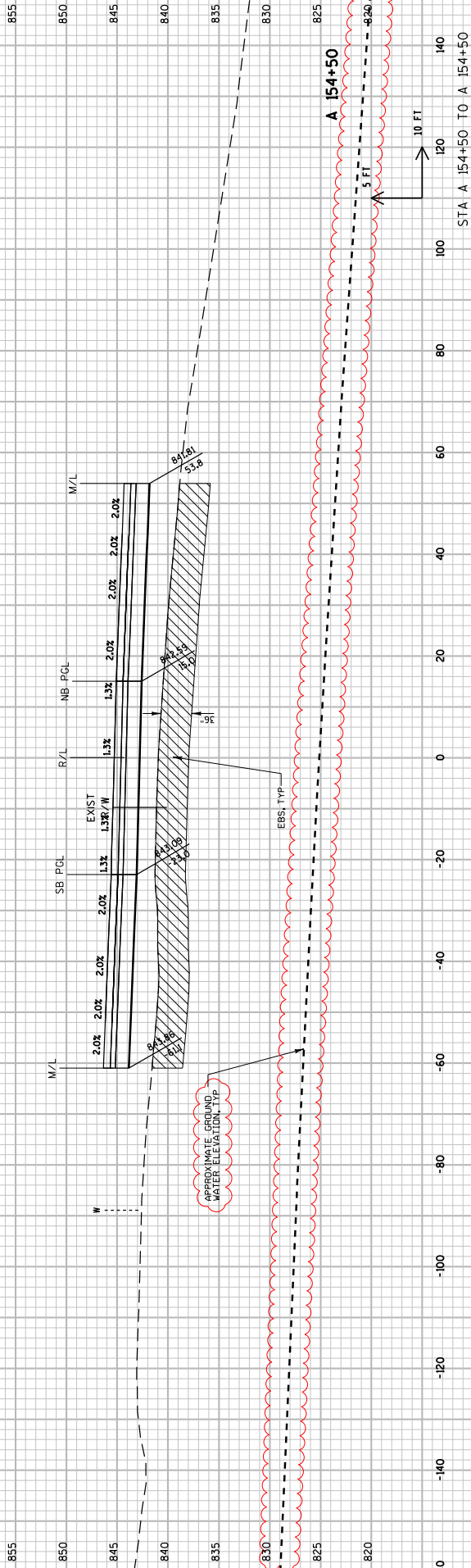
HWY: WAUKESHA BYPASS

SHEET 1550

WISDOT/CADD SHEET 21

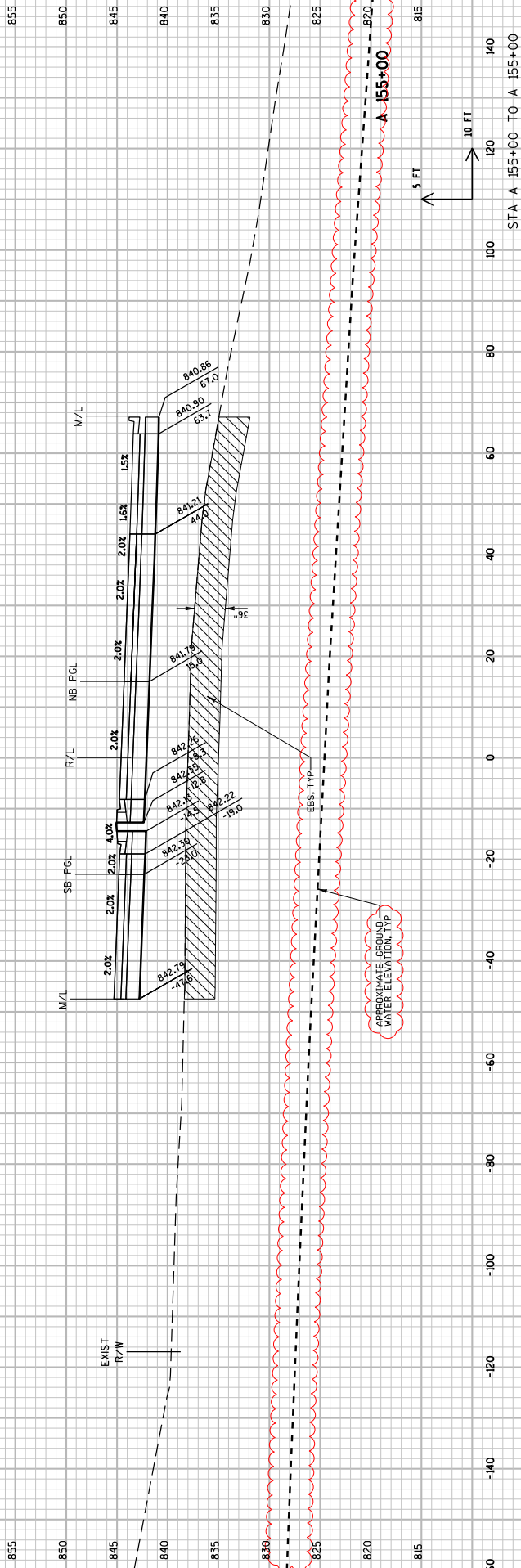
Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1551  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2B



Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1552  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2B



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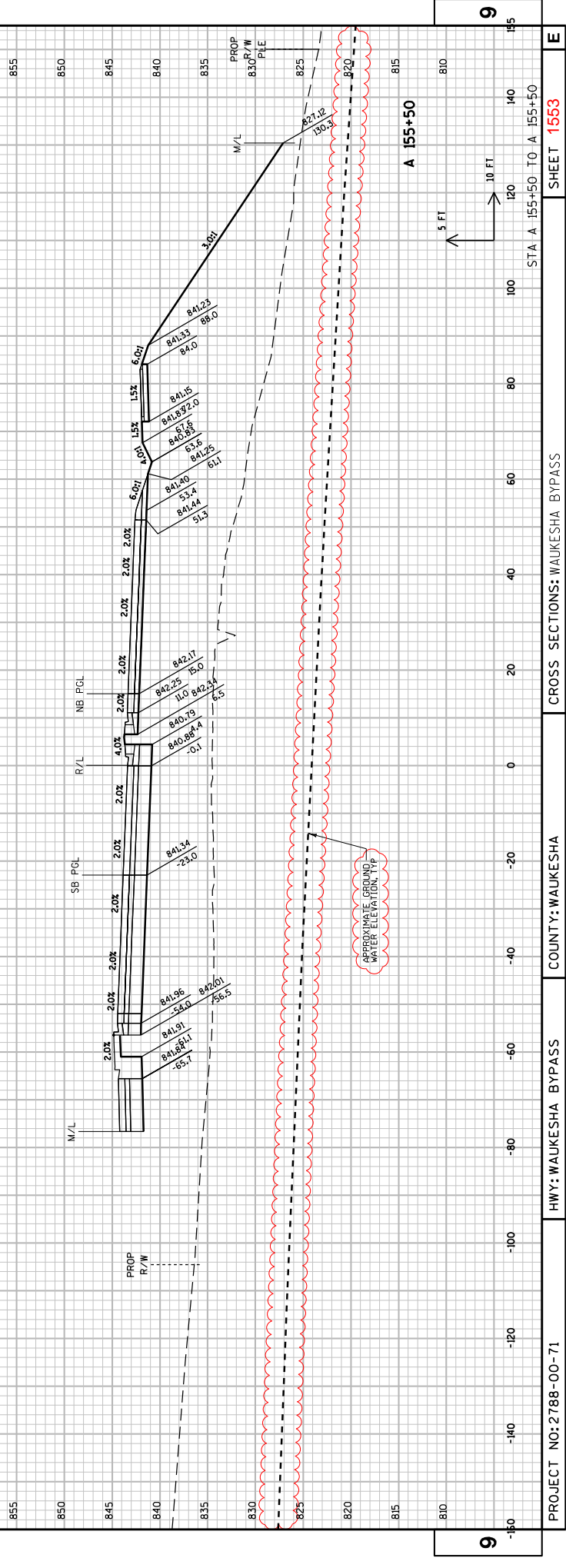
9

PROJECT NO: 2788-00-71  
 COUNTY: WAUKESHA  
 HWY: WAUKESHA BYPASS  
 CROSS SECTIONS: WAUKESHA BYPASS  
 SHEET 1552  
 STA A 155+00 TO A 155+00

FILE NAME : \\wplw00\lmgpr\proj\59971\1\cadd\by\_090201\_xs\_mb.dgn  
 PLOT BY : TFW.LP@WI  
 PLOT DATE : 6/26/2018 10:26:21 AM  
 PLOT SCALE : 20:1  
 WISDOT/CADD SHEET 21

ALL WORK COMPLETED IN STAGE 2B

Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1553  
July 2, 2018



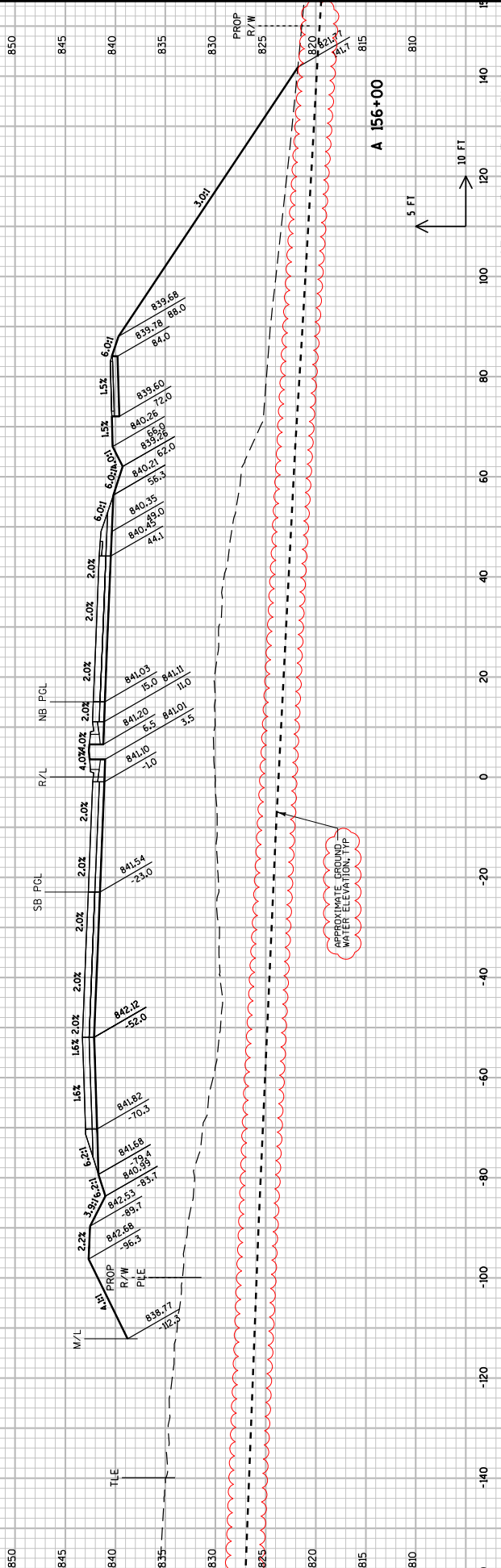
9

|                                                                  |                                                                  |                                                                  |                                                                  |                                                                  |     |     |     |     |     |    |    |    |     |     |     |     |
|------------------------------------------------------------------|------------------------------------------------------------------|------------------------------------------------------------------|------------------------------------------------------------------|------------------------------------------------------------------|-----|-----|-----|-----|-----|----|----|----|-----|-----|-----|-----|
| PROJECT NO: 2788-00-71                                           | HWY: WAUKESHA BYPASS                                             | COUNTY: WAUKESHA                                                 | CROSS SECTIONS: WAUKESHA BYPASS                                  | PLOT NAME: TWA11P61.Plot                                         |     |     |     |     |     |    |    |    |     |     |     |     |
| FILE NAME: \\FWL1w00\projr\proj\59971\1\ccds\by_090201.XS.MB.dgn | FILE NAME: \\FWL1w00\projr\proj\59971\1\ccds\by_090201.XS.MB.dgn | FILE NAME: \\FWL1w00\projr\proj\59971\1\ccds\by_090201.XS.MB.dgn | FILE NAME: \\FWL1w00\projr\proj\59971\1\ccds\by_090201.XS.MB.dgn | FILE NAME: \\FWL1w00\projr\proj\59971\1\ccds\by_090201.XS.MB.dgn |     |     |     |     |     |    |    |    |     |     |     |     |
| 855                                                              | 850                                                              | 845                                                              | 840                                                              | 835                                                              | 830 | 825 | 820 | 815 | 810 |    |    |    |     |     |     |     |
| -150                                                             | -140                                                             | -120                                                             | -100                                                             | -80                                                              | -60 | -40 | -20 | 0   | 20  | 40 | 60 | 80 | 100 | 120 | 140 | 150 |
| STA A 155+50 TO A 155+50                                         |                                                                  |                                                                  |                                                                  |                                                                  |     |     |     |     |     |    |    |    |     |     |     |     |
| SHEET 1553                                                       |                                                                  |                                                                  |                                                                  |                                                                  |     |     |     |     |     |    |    |    |     |     |     |     |
| E                                                                |                                                                  |                                                                  |                                                                  |                                                                  |     |     |     |     |     |    |    |    |     |     |     |     |
| WISDOT/CADD SHEET 21                                             |                                                                  |                                                                  |                                                                  |                                                                  |     |     |     |     |     |    |    |    |     |     |     |     |

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Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1554  
July 2, 2018

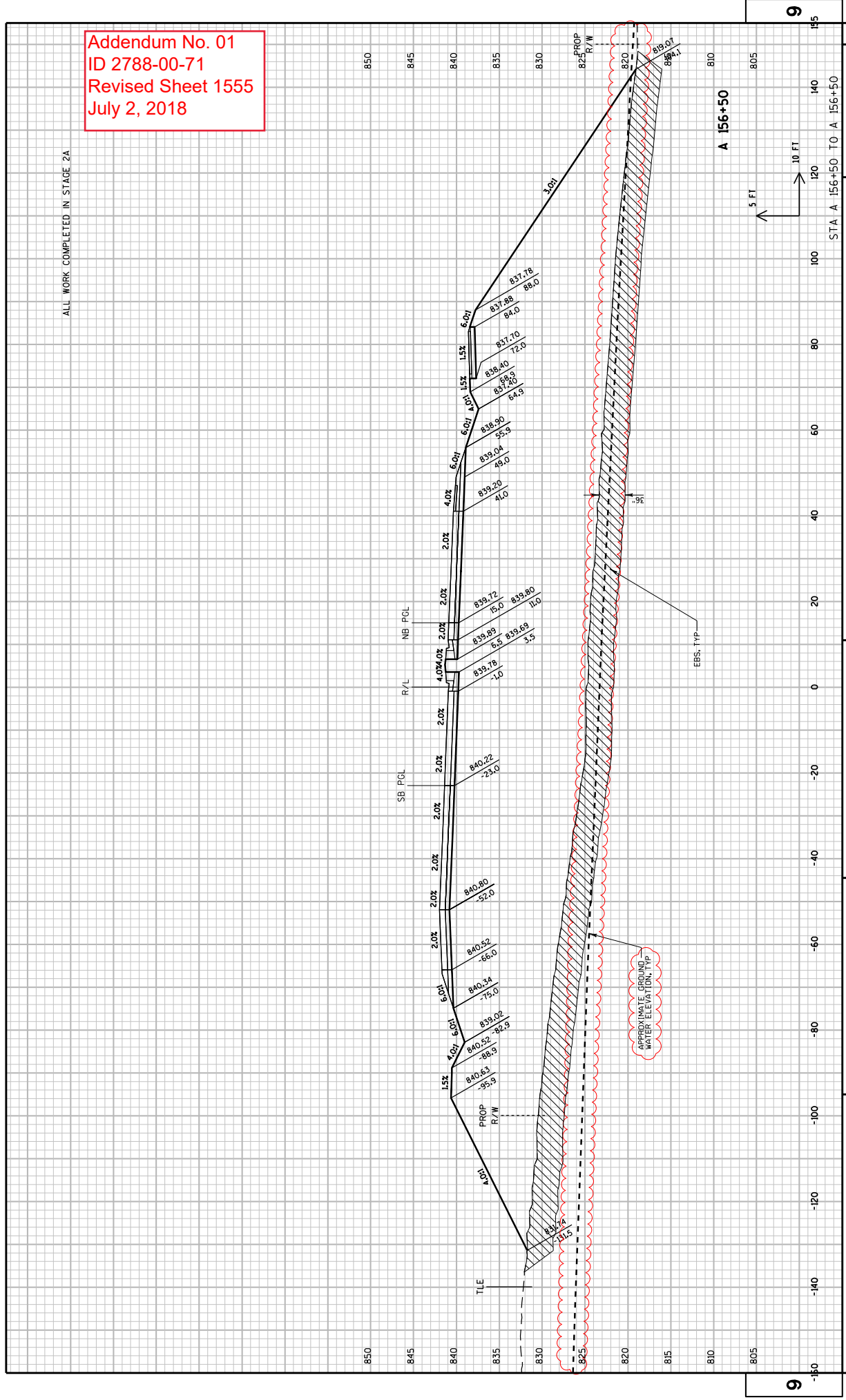
ALL WORK COMPLETED IN STAGE 2A



PROJECT NO: 2788-00-71 COUNTY: WAUKESHA HWY: WAUKESHA BYPASS CROSS SECTIONS: WAUKESHA BYPASS SHEET 1554 E  
FILE NAME : \\w11w001\mgr\proj\59971\1\cadd\wy\_090201\_xs\_wb.dgn PLOT DATE : 6/26/2018 10:26:22 AM PLOT BY : TFW.LP@lin PLOT SCALE : 20:1 WISDOT/CADD SHEET 21

Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1555  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A



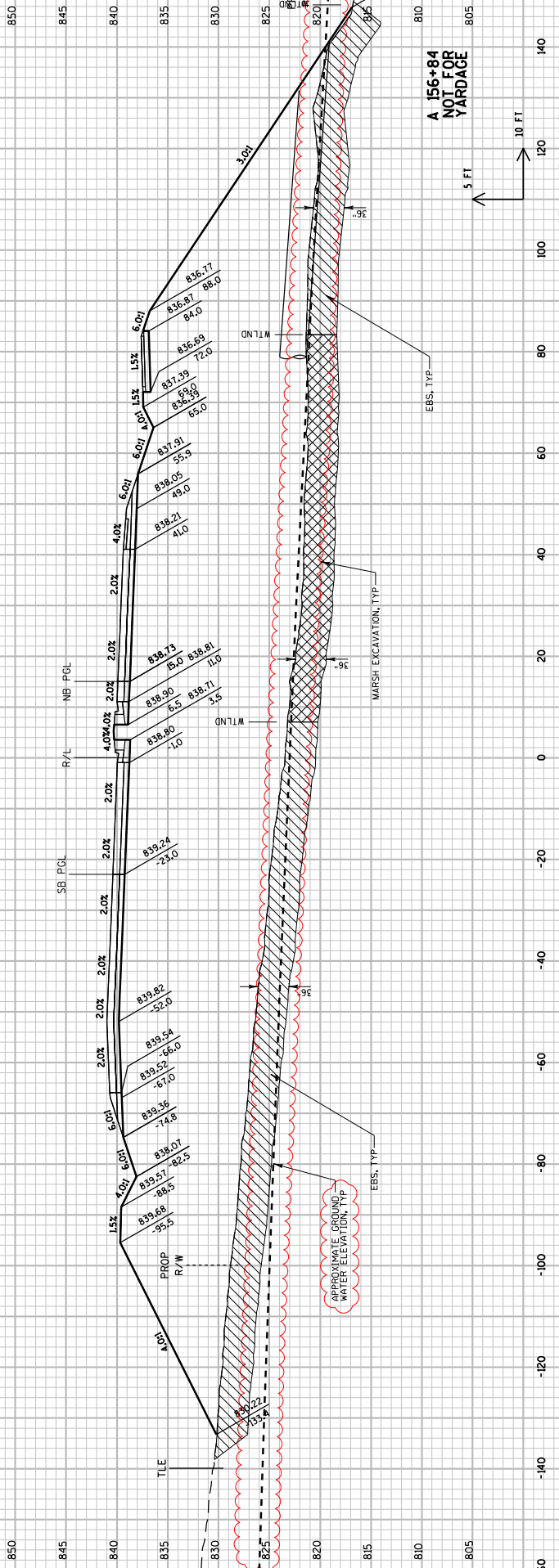
PROJECT NO: 2788-00-71  
 COUNTY: WAUKESHA  
 HWY: WAUKESHA BYPASS  
 CROSS SECTIONS: WAUKESHA BYPASS  
 SHEET 1555  
 STA A 156+50 TO A 156+50

FILE NAME : \\nw11w00\lmp\proj\59971\1\cadd\by\_090201\_xs\_mb.dgn  
 PLOT BY : TFW.LP@i.in  
 PLOT DATE : 6/26/2018 10:26:23 AM  
 PLOT SCALE : 20:1  
 WISDOT/CADD SHEET 21



Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1556  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A



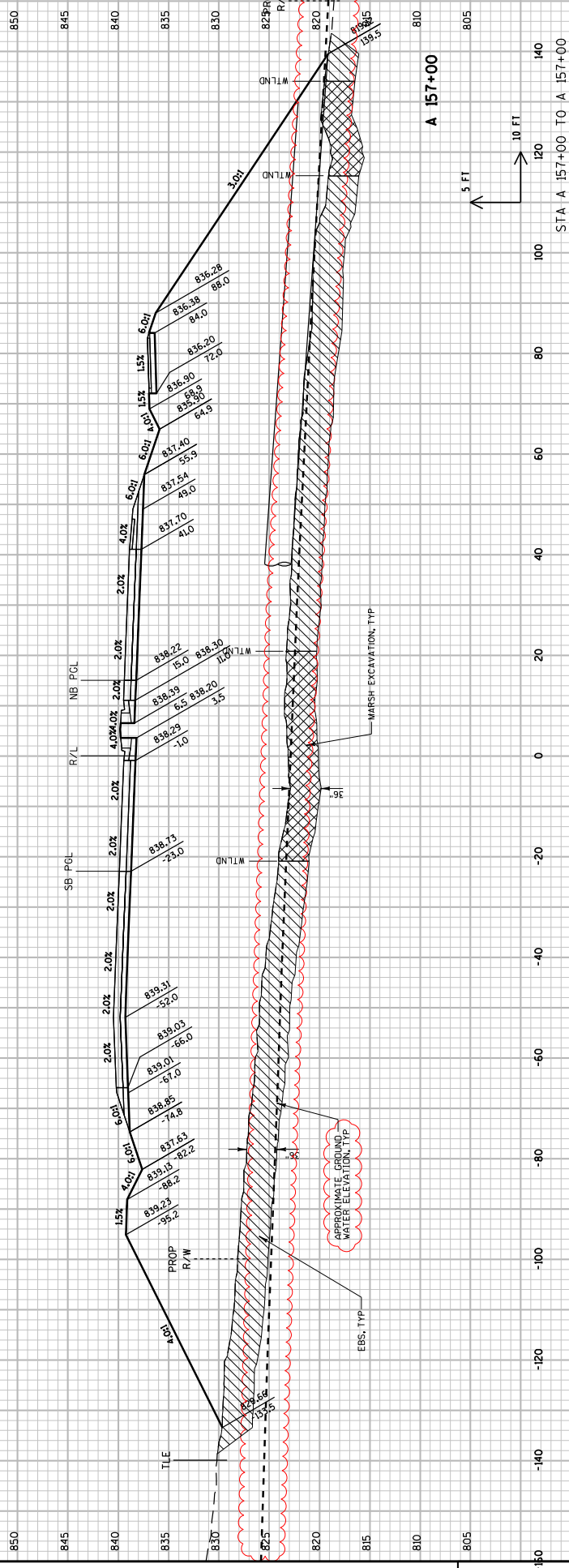
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PROJECT NO: 2788-00-71 COUNTY: WAUKESHA CROSS SECTIONS: WAUKESHA BYPASS SHEET 1556  
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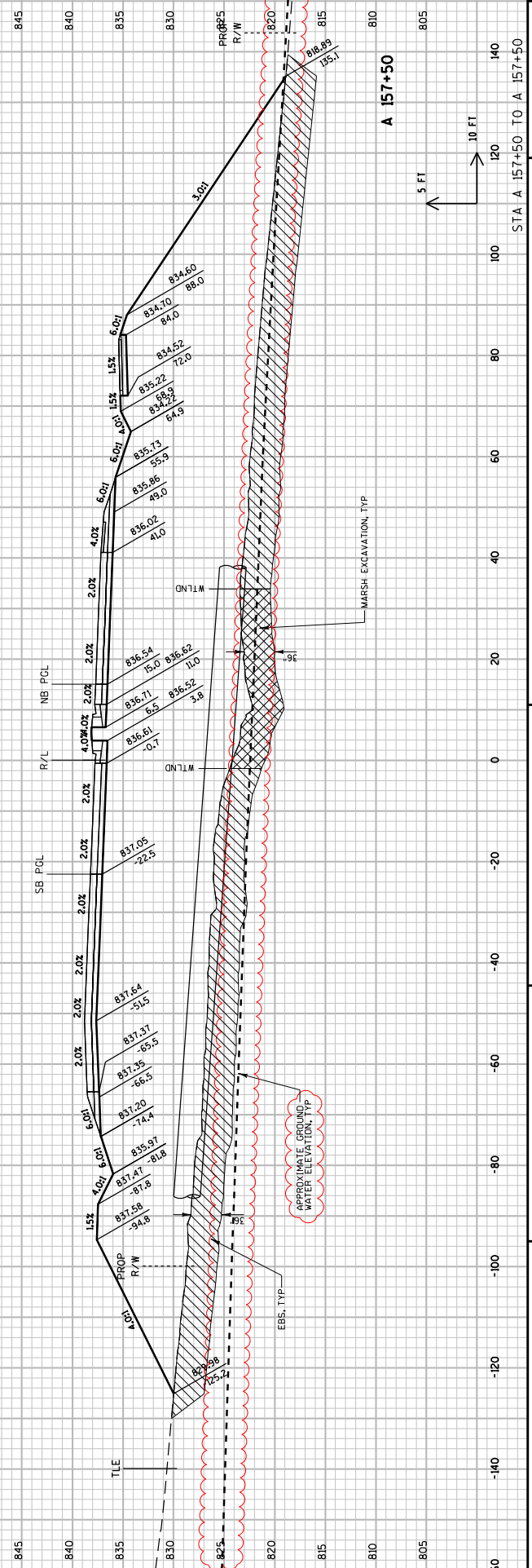
ALL WORK COMPLETED IN STAGE 2A

Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1557  
July 2, 2018



ALL WORK COMPLETED IN STAGE 2A

Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1558  
July 2, 2018



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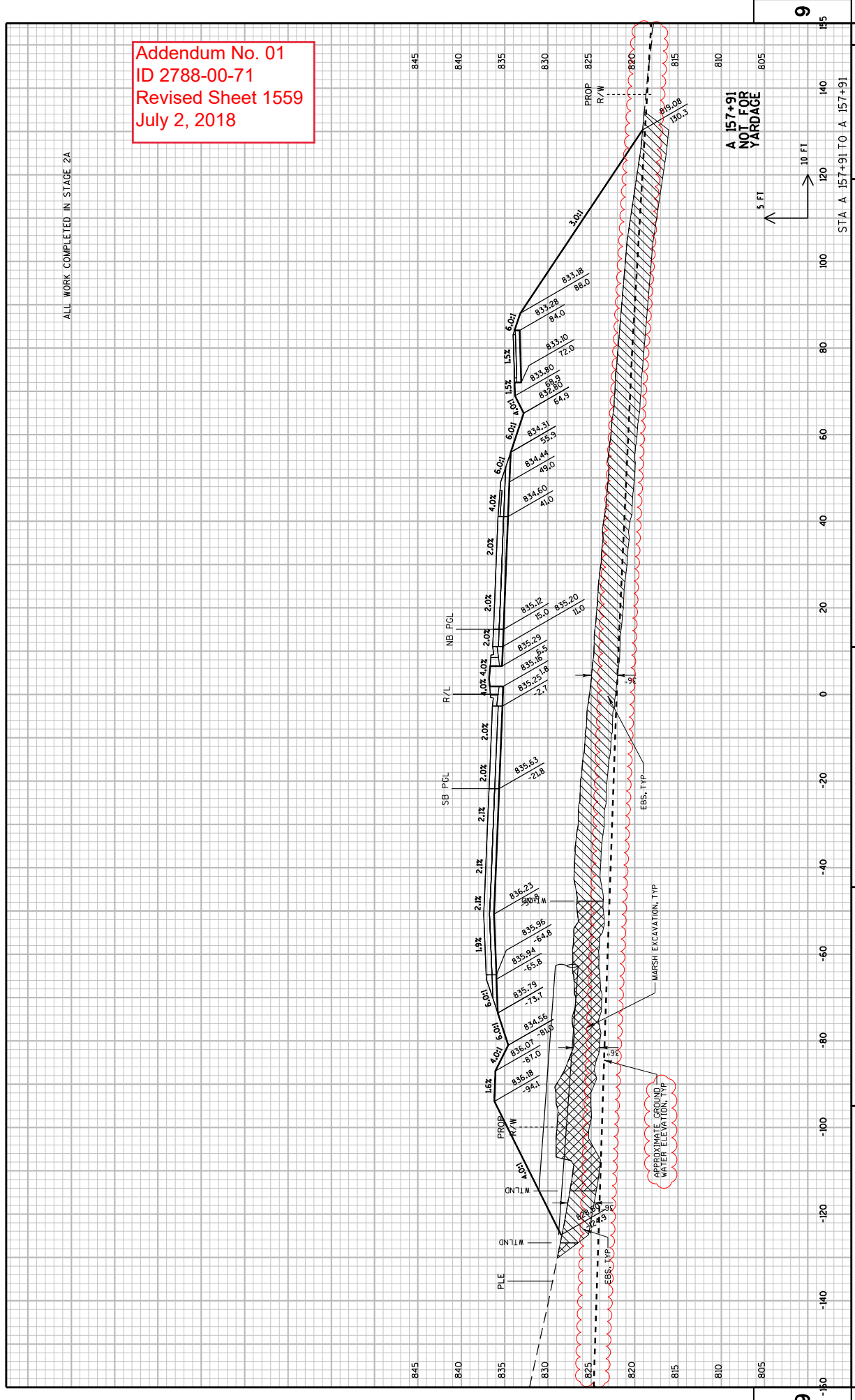
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STA A 157+50 TO A 157+50

Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1559  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A



Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1560  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A

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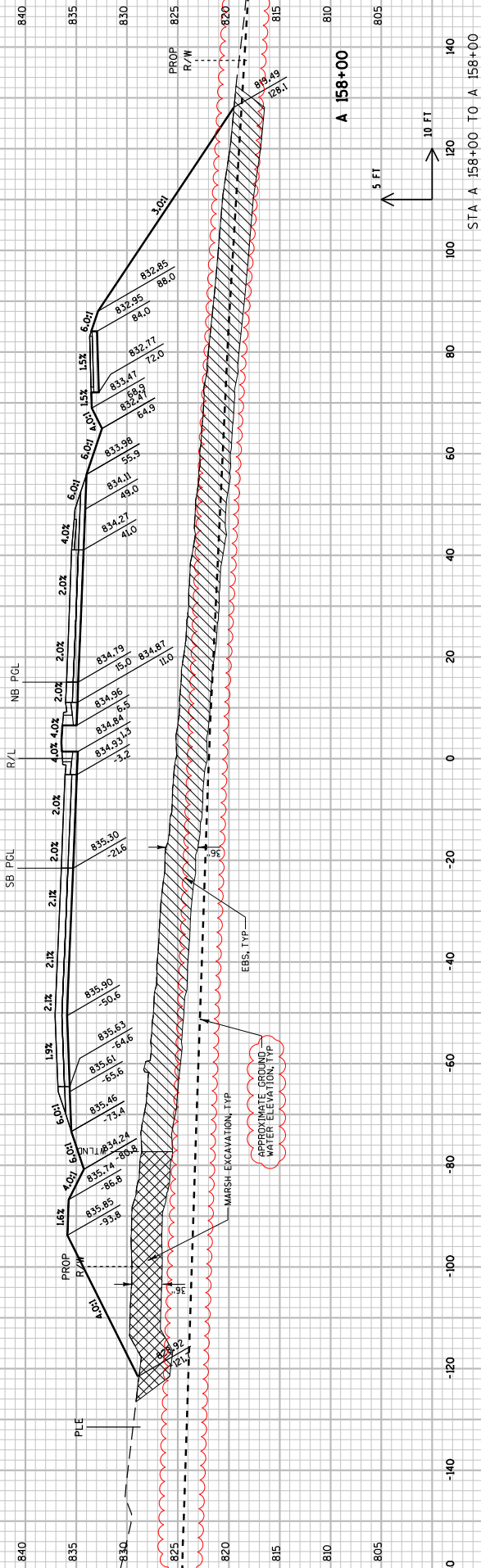
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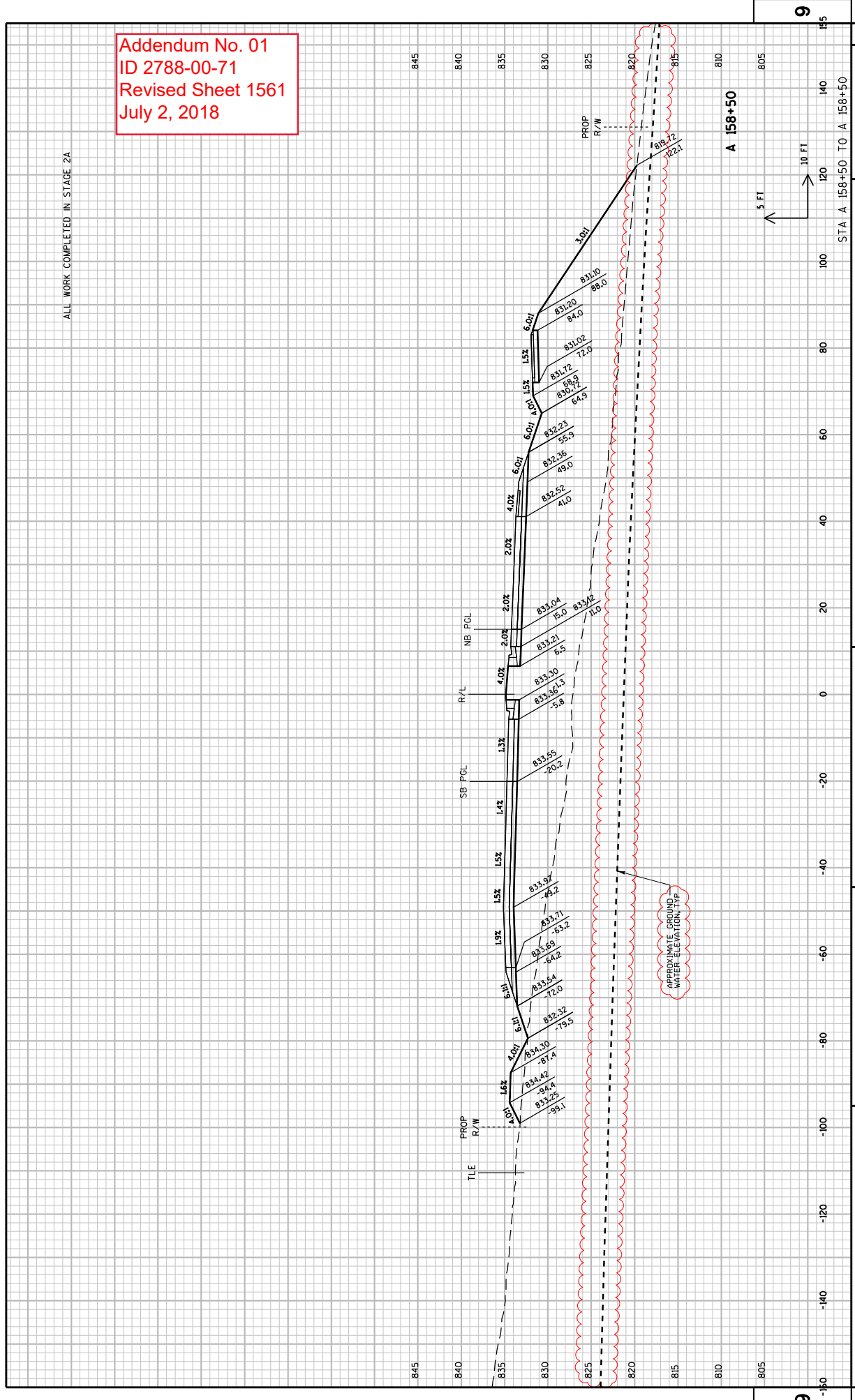
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PROJECT NO: 2788-00-71 COUNTY: WAUKESHA CROSS SECTIONS: WAUKESHA BYPASS SHEET 1560 E

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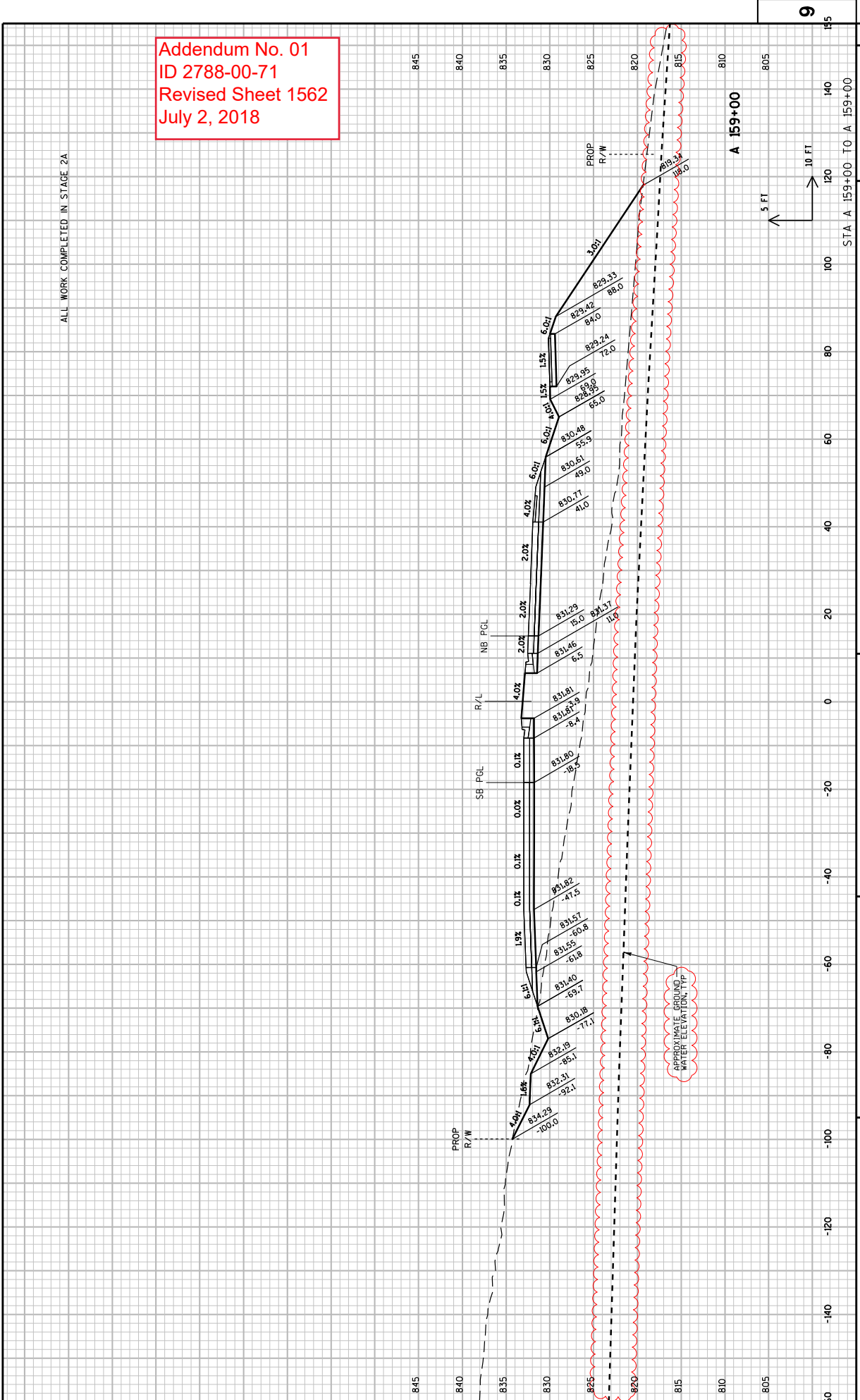
Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1561  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A



Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1562  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A



PROJECT NO: 2788-00-71

COUNTY: WAUKESHA

CROSS SECTIONS: WAUKESHA BYPASS

SHEET 1562

STA. A 159+00 TO A 159+00

FILE NAME : \\nw11w00\jmgr\proj\59971\1\cadd\by\_090201\_xs\_wb.dgn

PLOT BY : fhw.lj@wis.edu

PLOT DATE : 6/26/2018 10:26:31 AM

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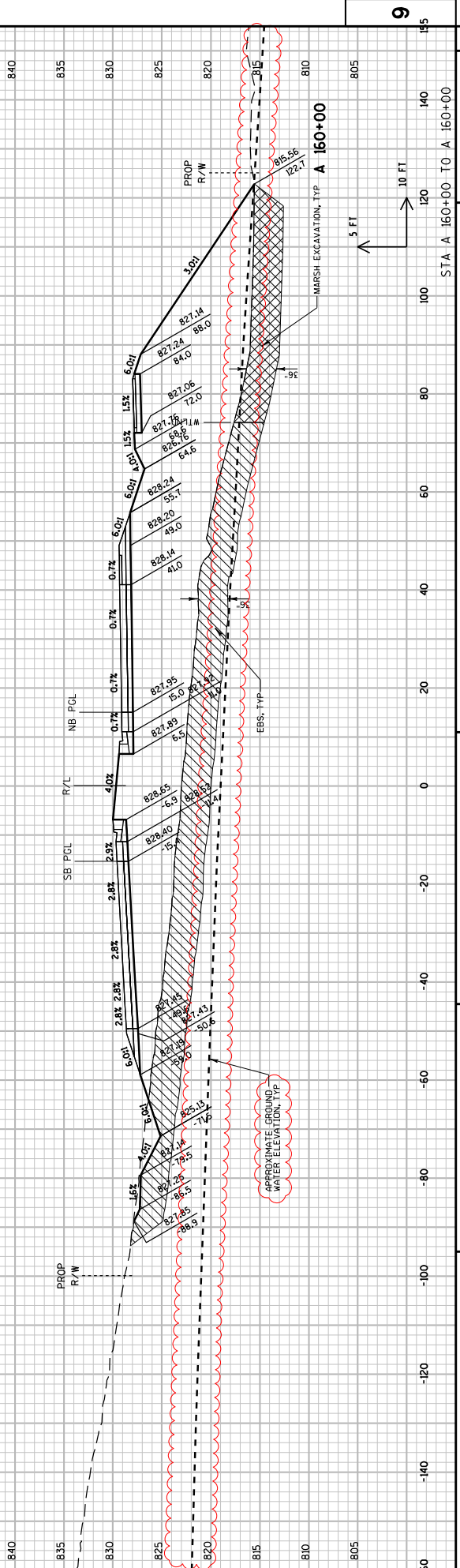
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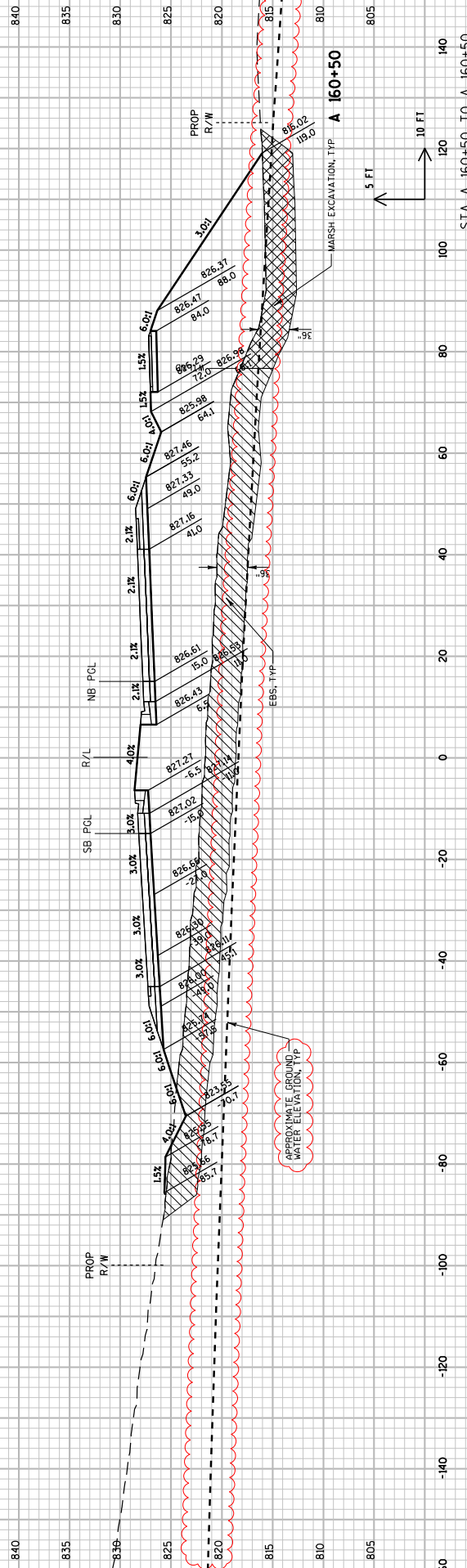
Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1564  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A



Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1565  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A



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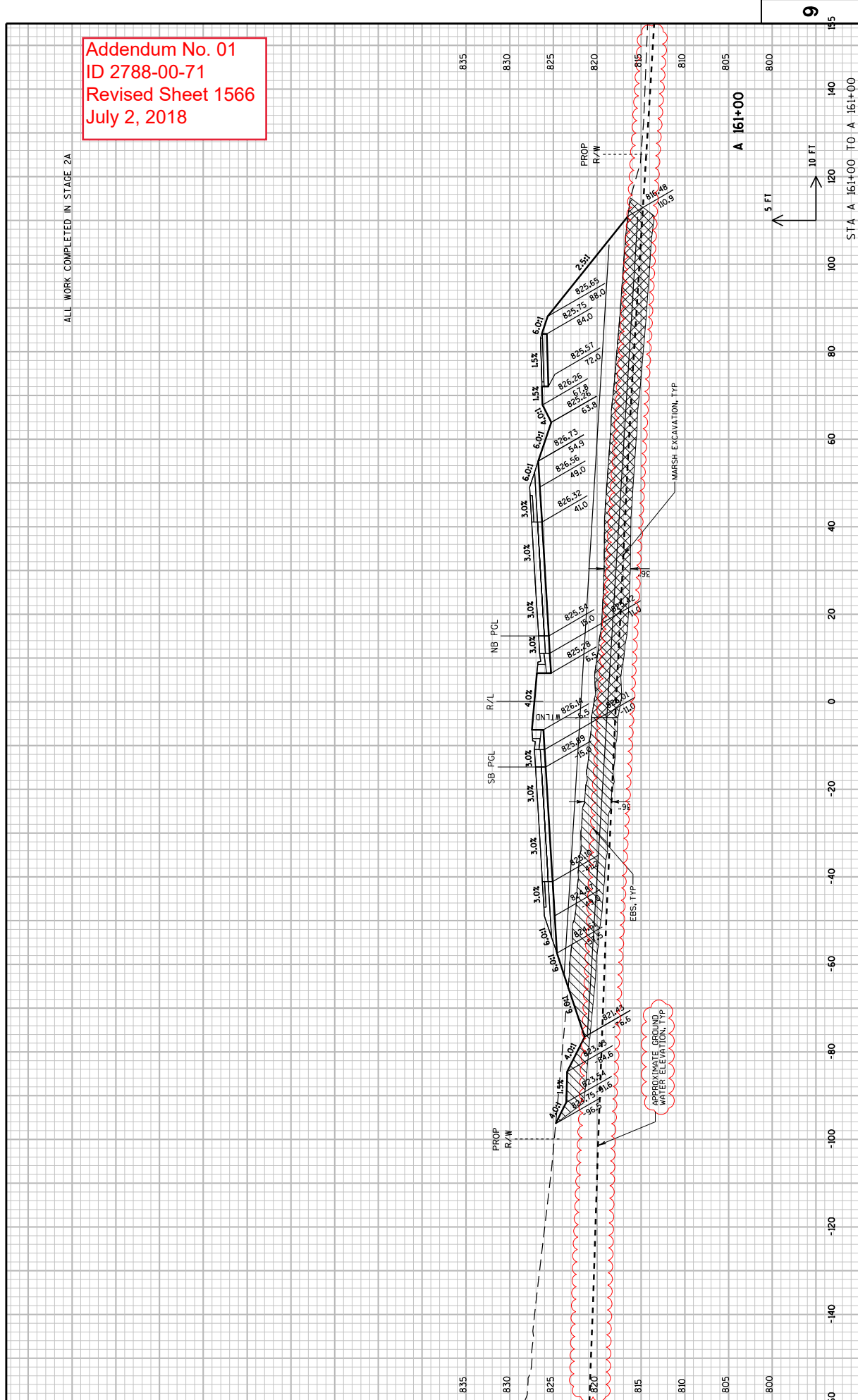
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PROJECT NO: 2788-00-71  
 COUNTY: WAUKESHA  
 HWY: WAUKESHA BYPASS  
 CROSS SECTIONS: WAUKESHA BYPASS  
 SHEET 1565  
 STA A 160+50 TO A 160+50

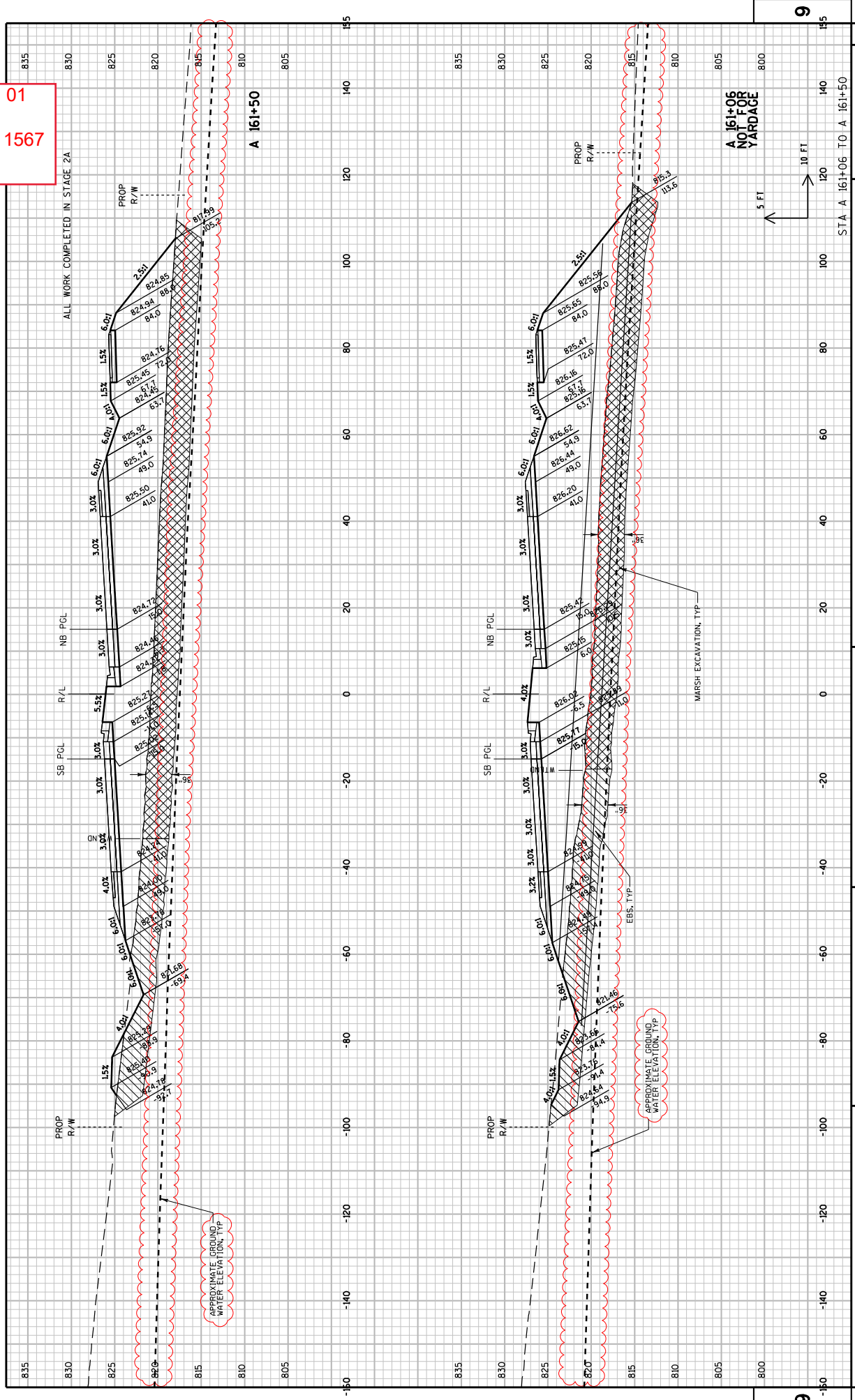
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 PLOT SCALE : 20:1  
 WISDOT/CADD SHEET 21

Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1566  
 July 2, 2018

ALL WORK COMPLETED IN STAGE 2A



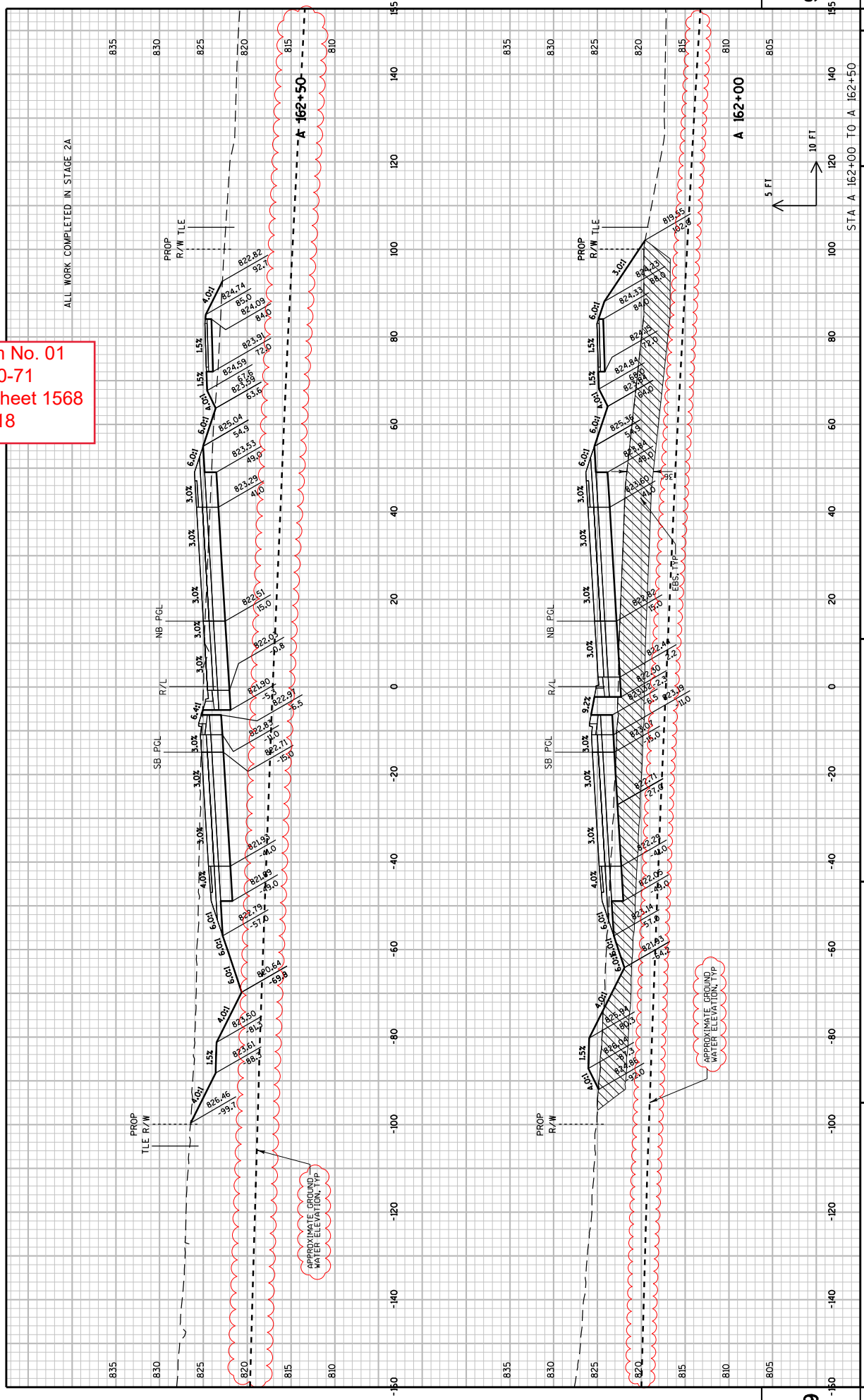
Addendum No. 01  
ID 2788-00-71  
Revised Sheet 1567  
July 2, 2018



PROJECT NO: 2788-00-71  
COUNTY: WAUKESHA  
HWY: WAUKESHA BYPASS  
CROSS SECTIONS: WAUKESHA BYPASS  
SHEET 1567  
E

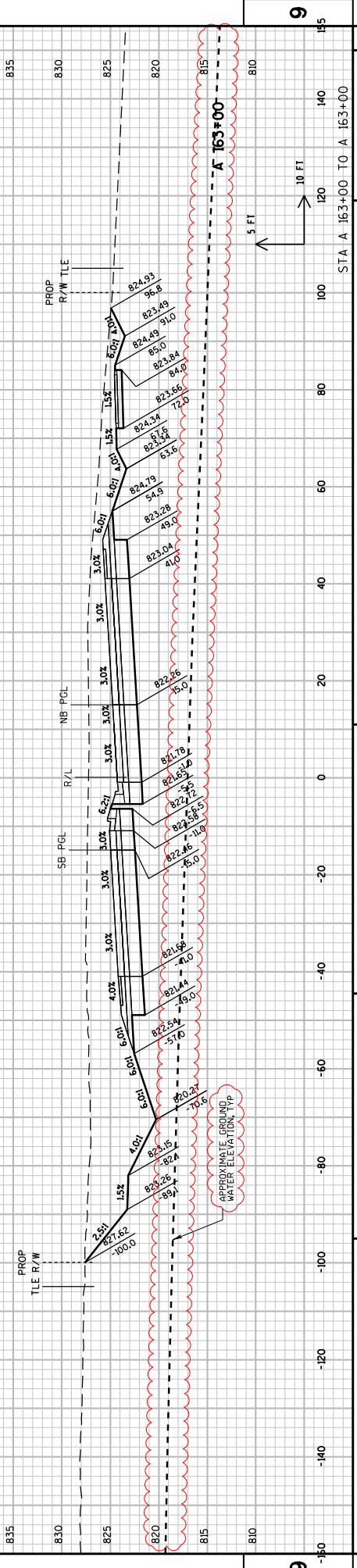
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PLOT BY : TWL\PLM\JM  
PLOT SCALE : 20:1  
WISDOT/CADD SHEET 21

Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1568  
 July 2, 2018

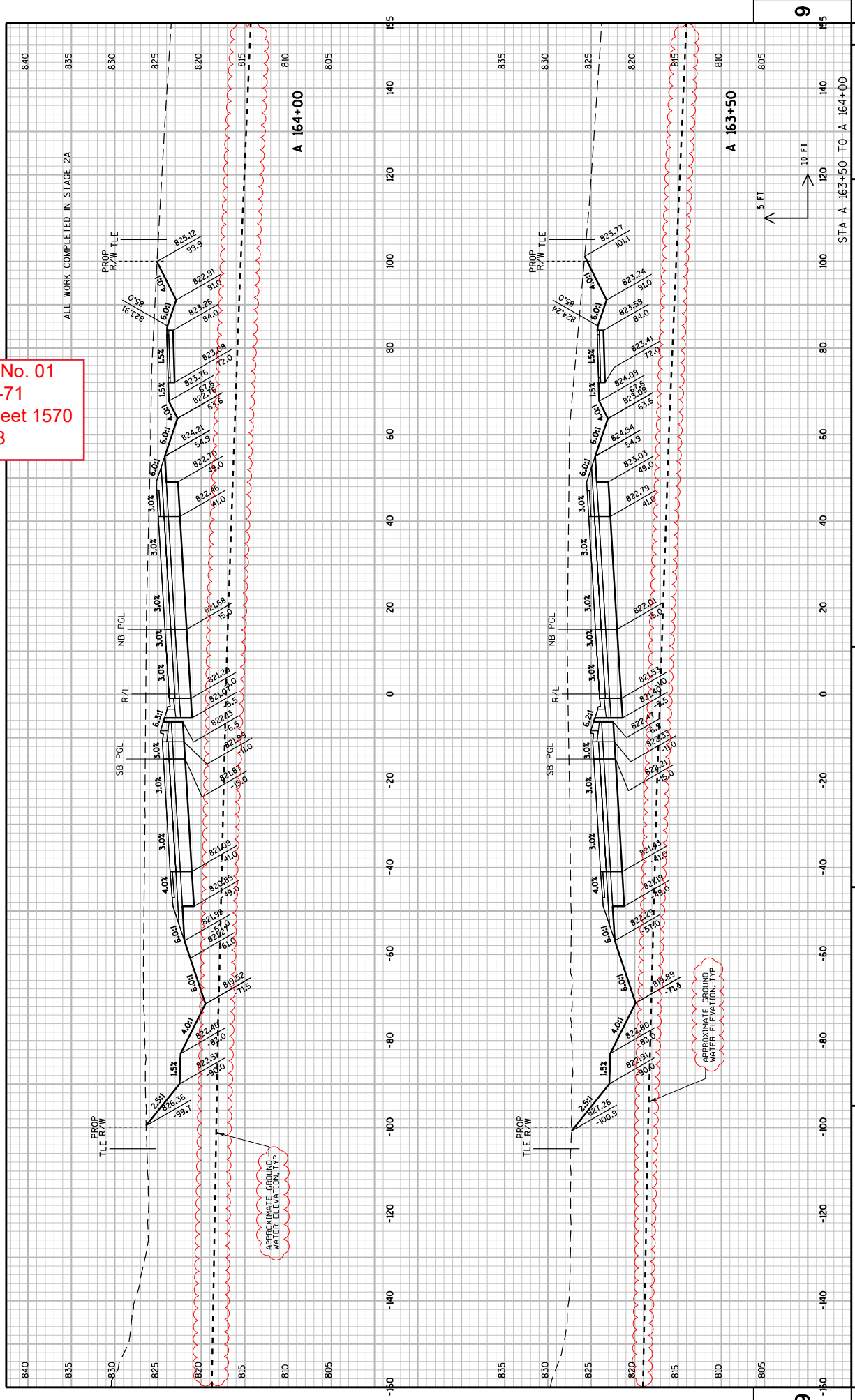


Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1569  
 July 2, 2018

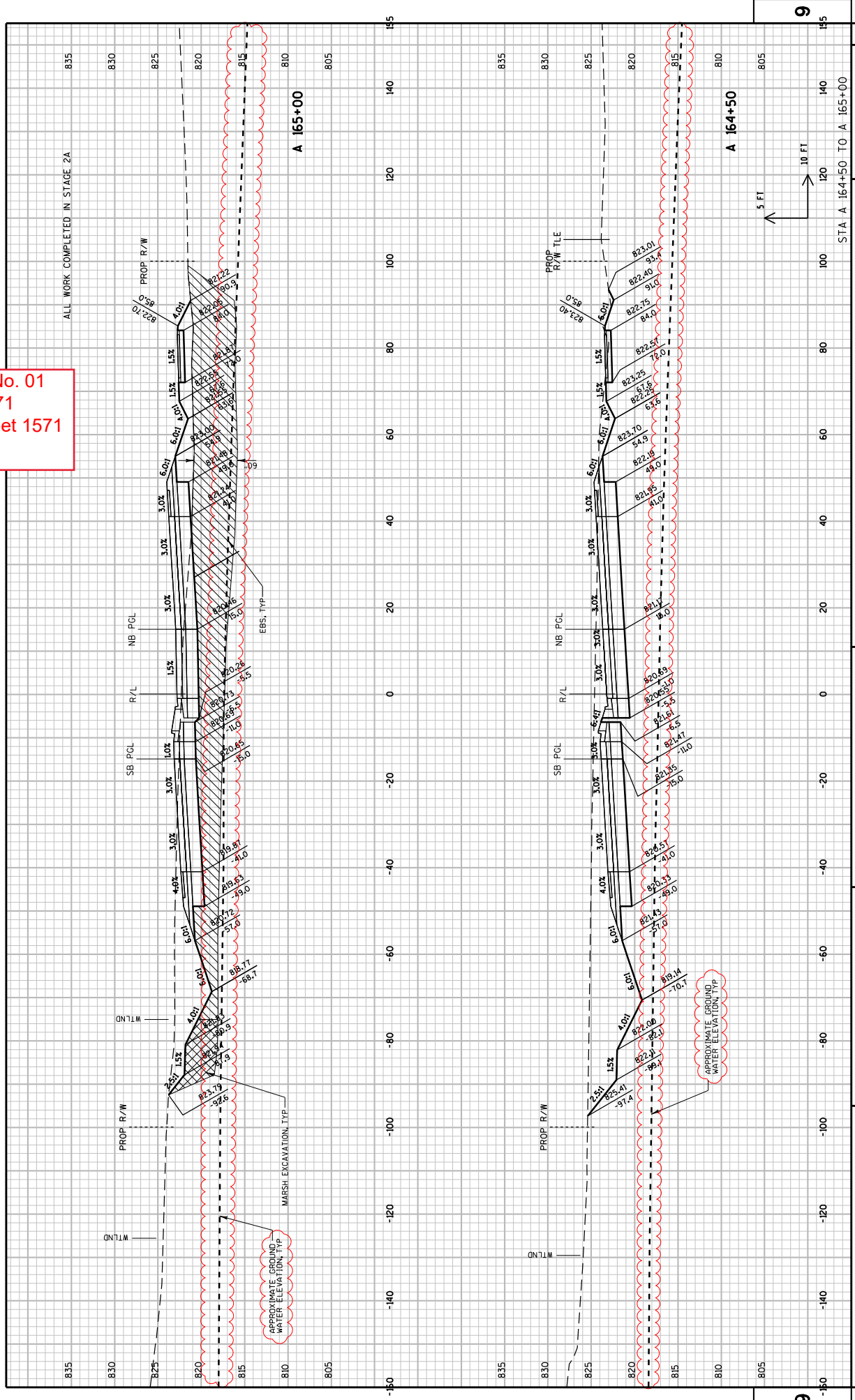
ALL WORK COMPLETED IN STAGE 2A



Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1570  
 July 2, 2018



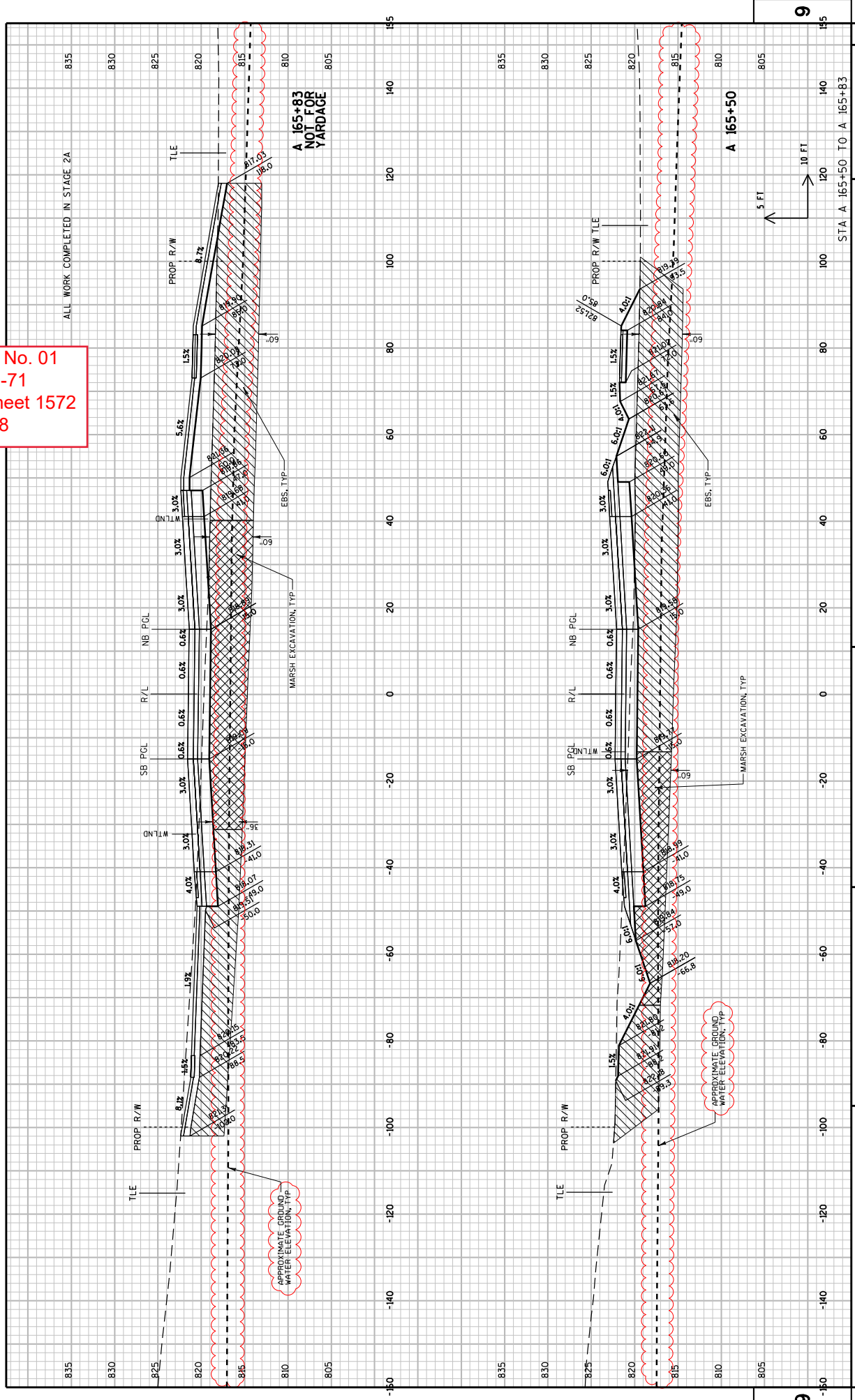
Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1571  
 July 2, 2018



ALL WORK COMPLETED IN STAGE 2A



Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1572  
 July 2, 2018



ALL WORK COMPLETED IN STAGE 2A

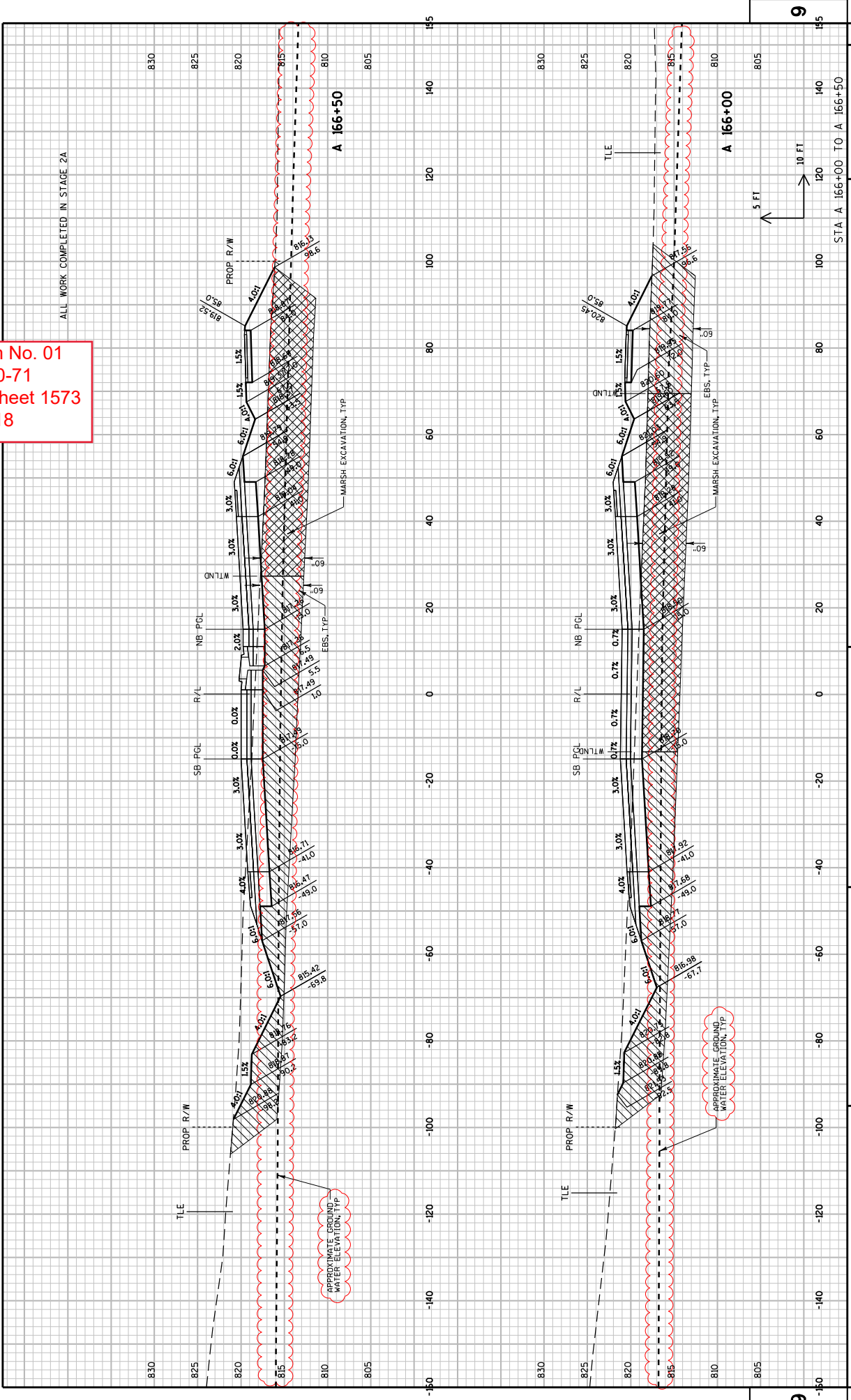
A 165+83  
 NOT FOR  
 YARDAGE

APPROXIMATE GROUND  
 WATER ELEVATION, TYP

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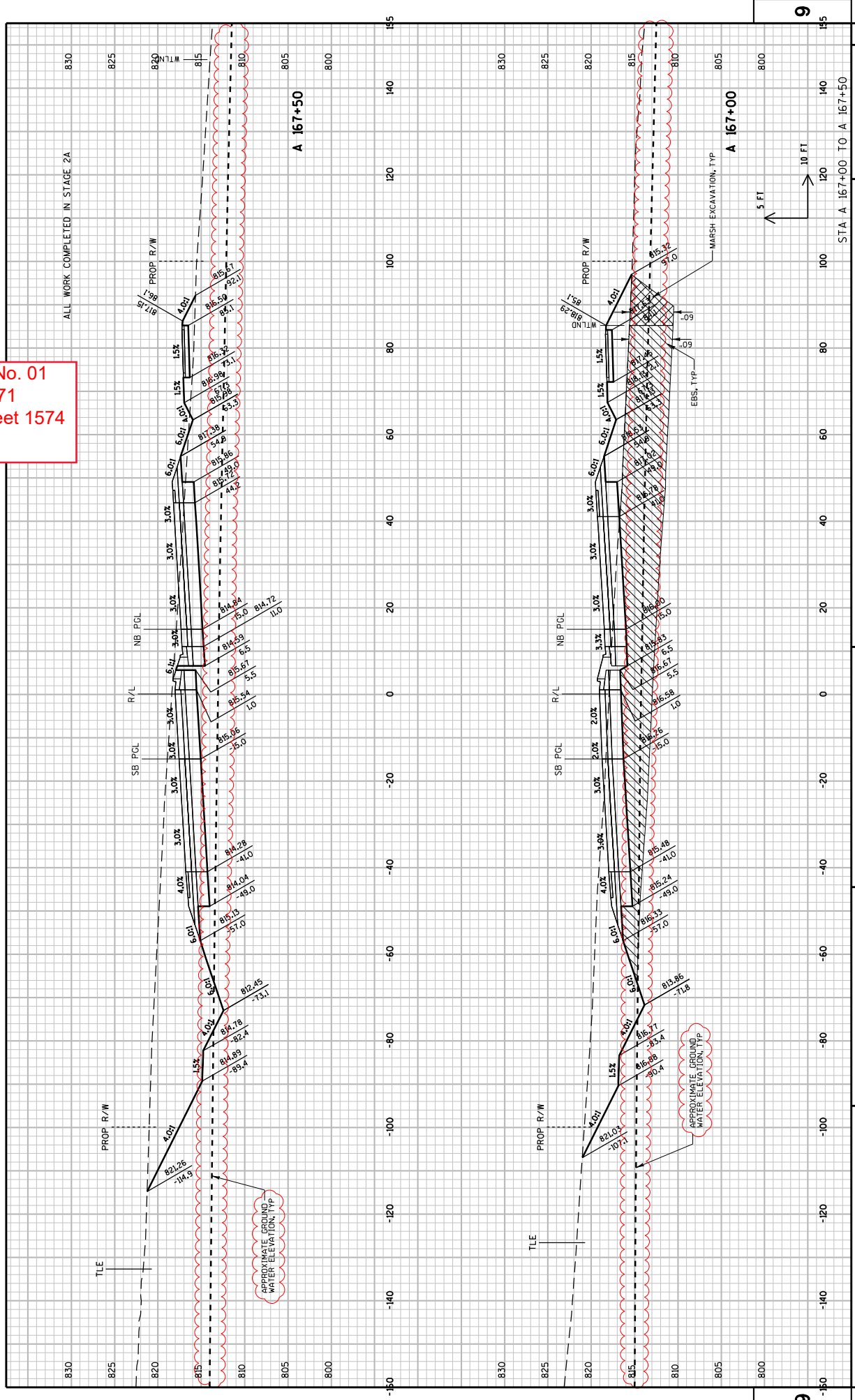
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Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1573  
 July 2, 2018

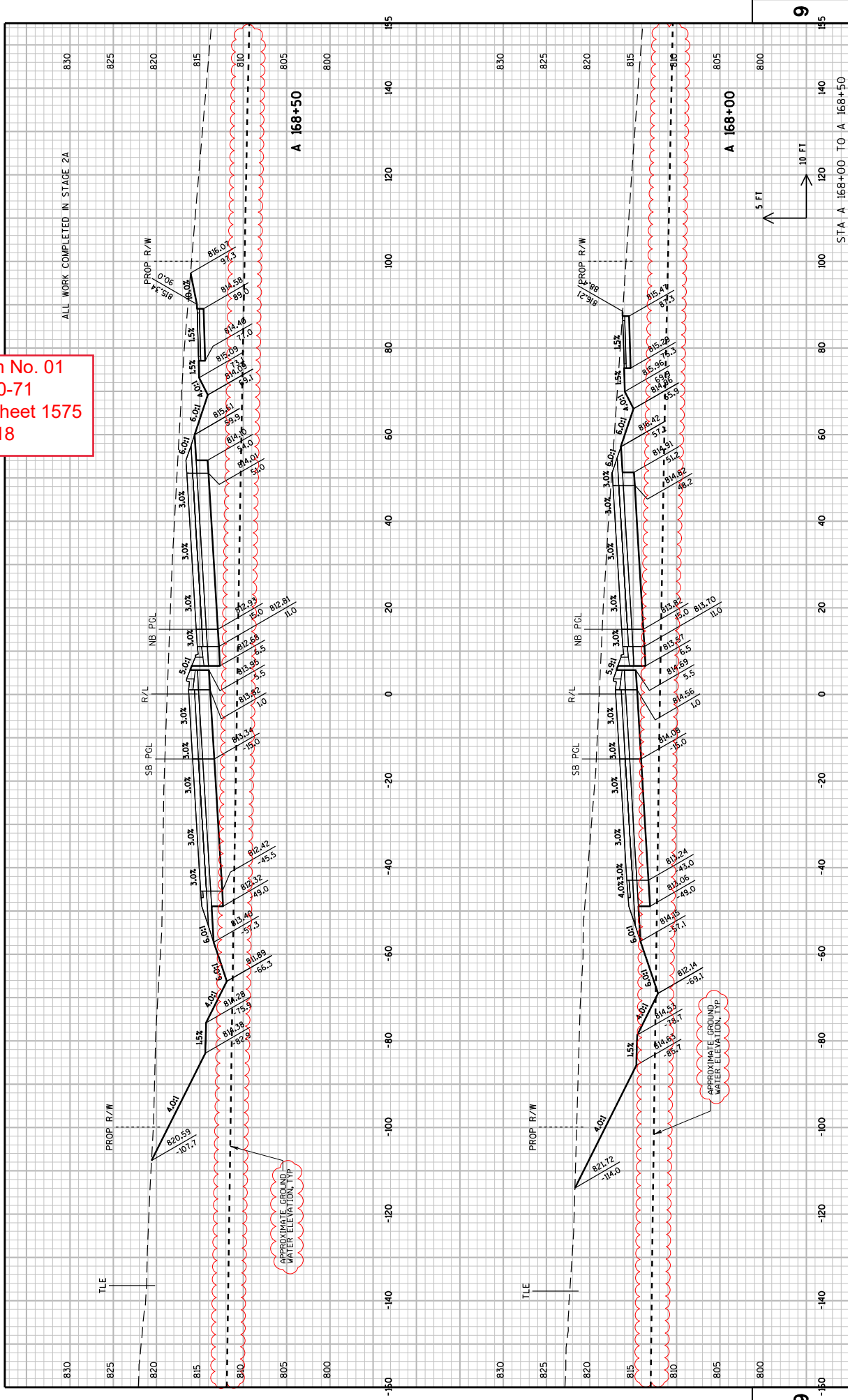


ALL WORK COMPLETED IN STAGE 2A

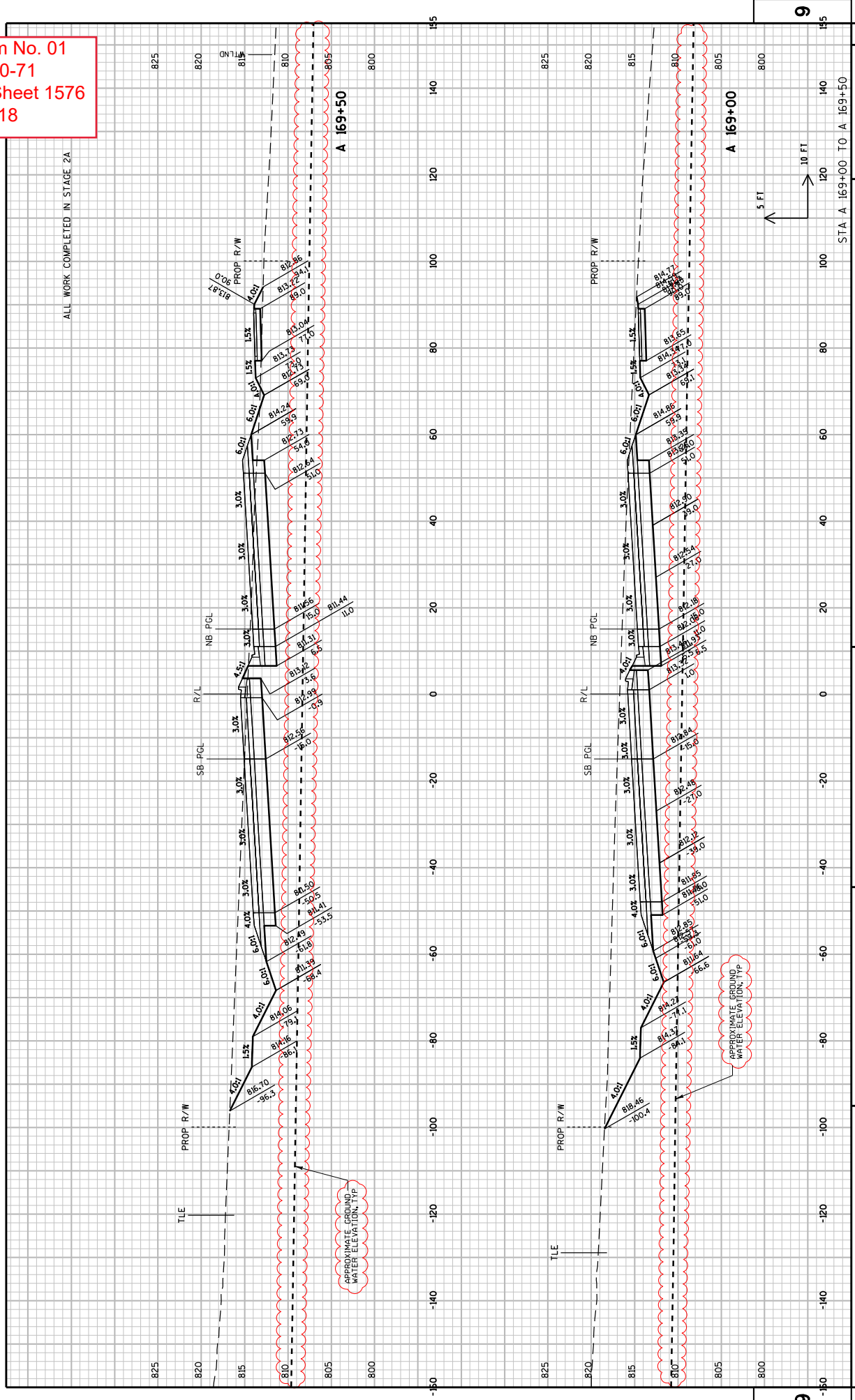
Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1574  
 July 2, 2018



Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1575  
 July 2, 2018



Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1576  
 July 2, 2018

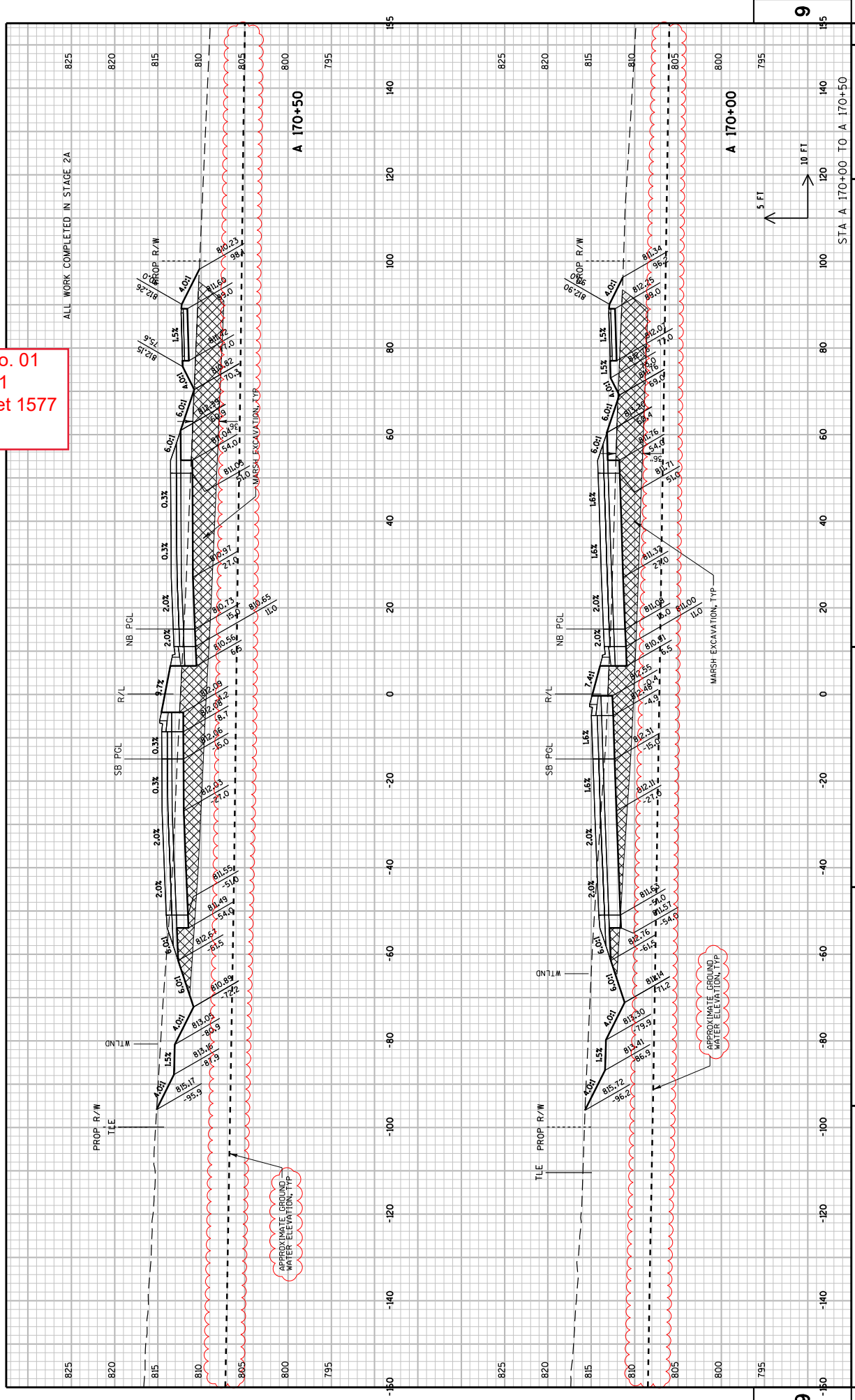


ALL WORK COMPLETED IN STAGE 2A

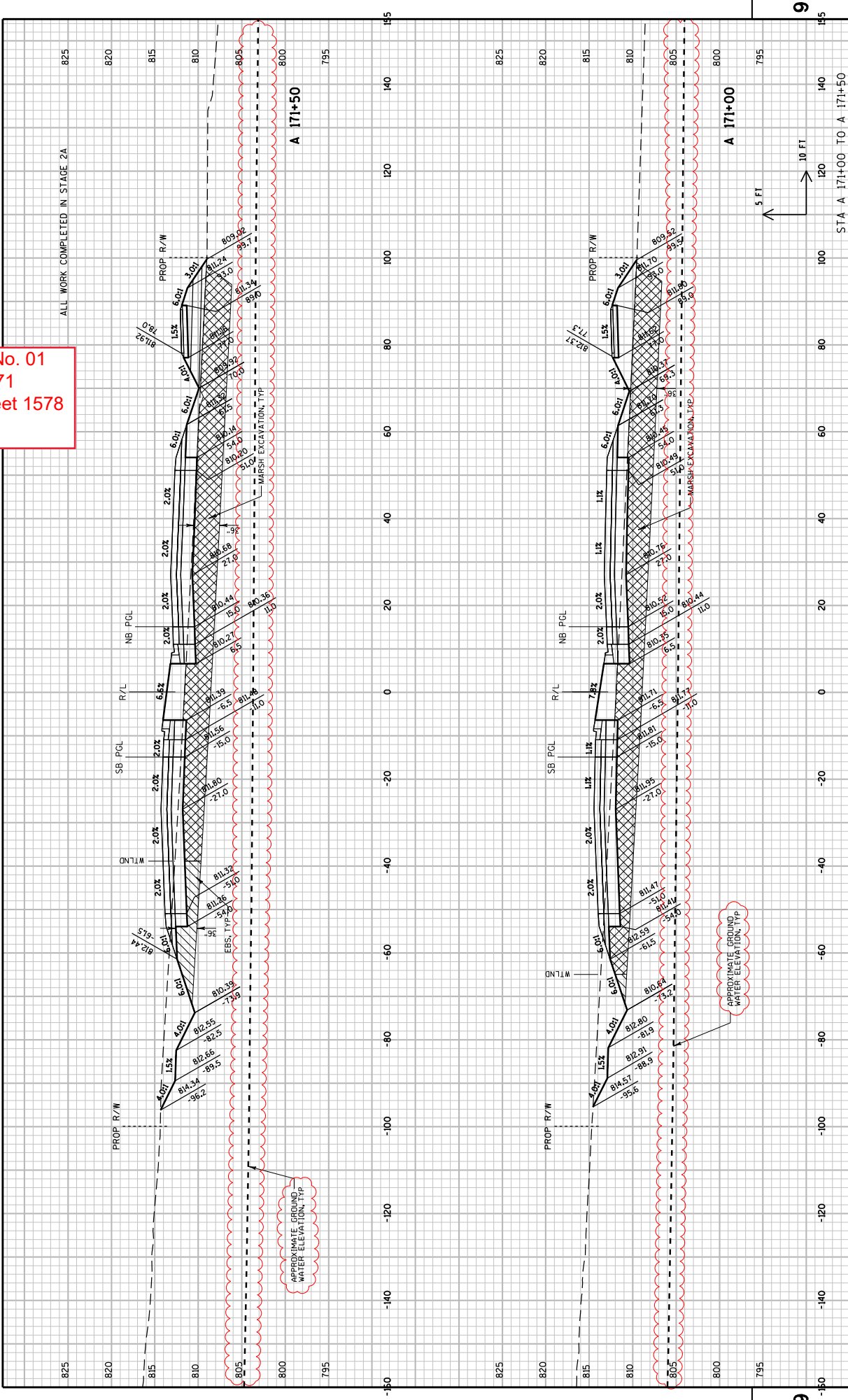
PROJECT NO: 2788-00-71  
 COUNTY: WAUKESHA  
 CROSS SECTIONS: WAUKESHA BYPASS  
 SHEET 1576  
 STA A 169+00 TO A 169+50

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 PLOT BY : TWS\JLH  
 PLOT SCALE : 20:1  
 WISDOT/CADD SHEET 21

Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1577  
 July 2, 2018



**Addendum No. 01**  
**ID 2788-00-71**  
**Revised Sheet 1578**  
**July 2, 2018**



ALL WORK COMPLETED IN STAGE 2A

EXISTING GROUND  
 WATER ELEVATION (FT)

EXISTING GROUND  
 WATER ELEVATION (FT)

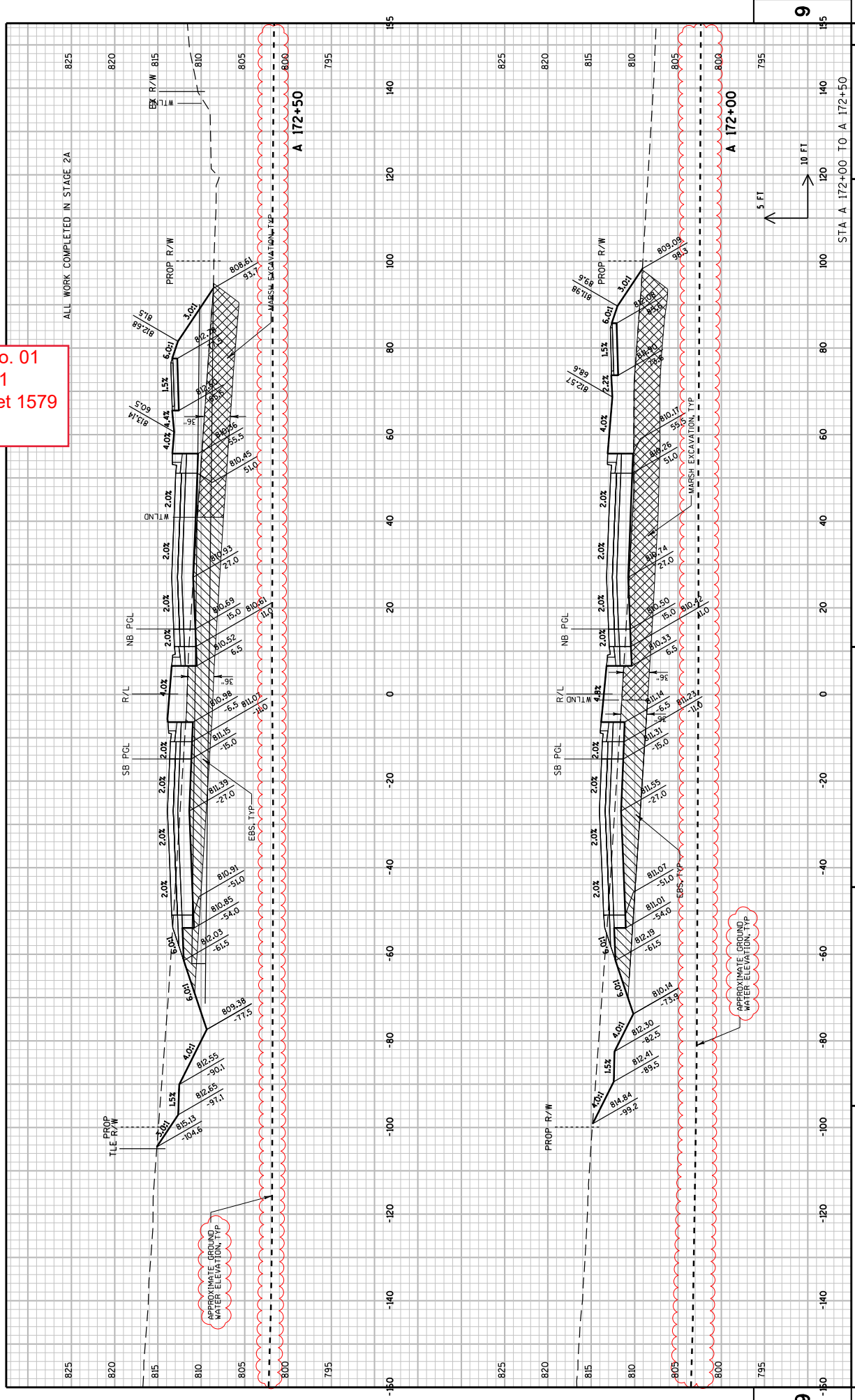
5 FT  
 10 FT

STA A 171+00 TO A 171+50

PROJECT NO: 2788-00-71 COUNTY: WAUKESHA HWY: WAUKESHA BYPASS CROSS SECTIONS: WAUKESHA BYPASS SHEET 1578 E

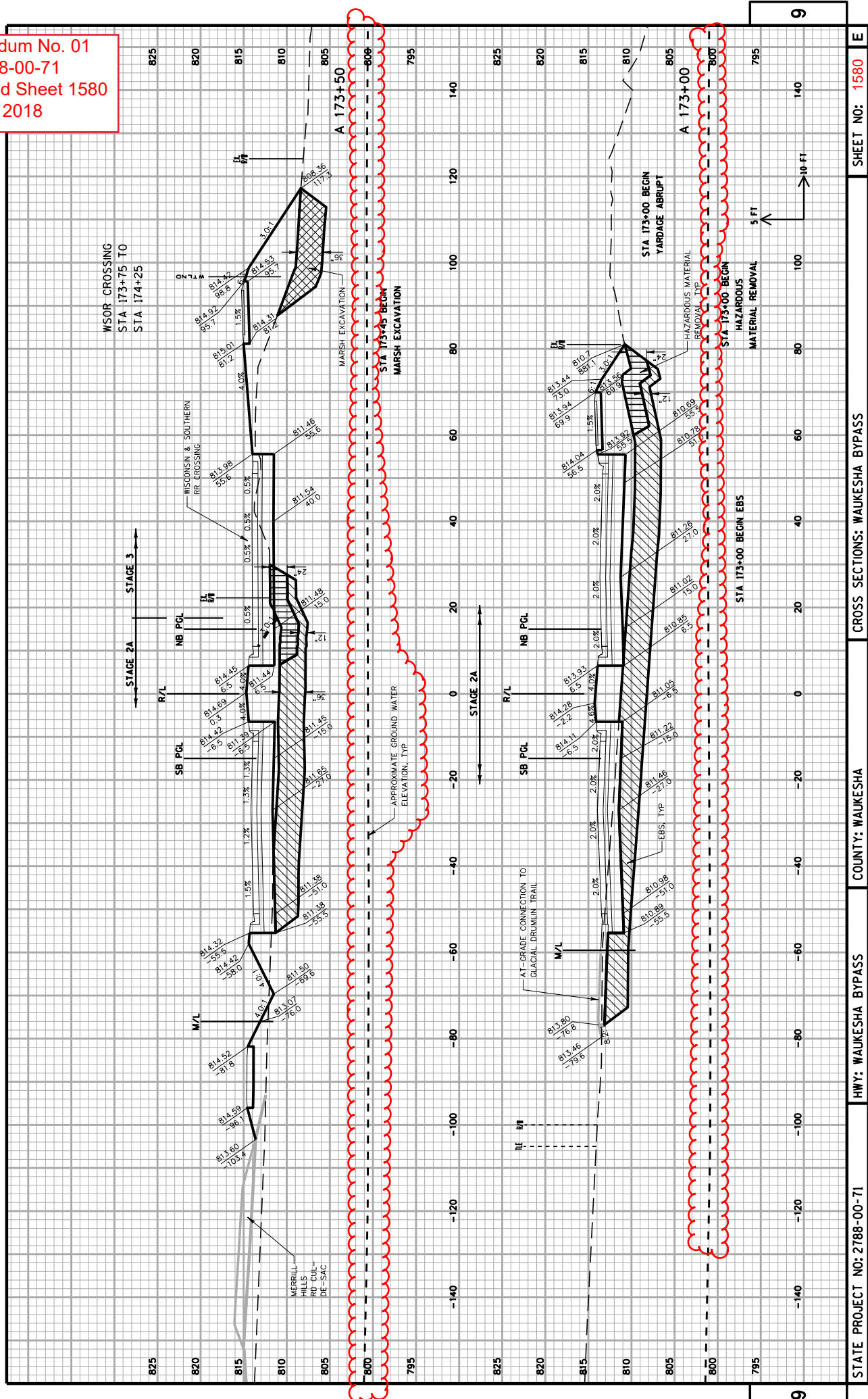
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Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1579  
 July 2, 2018





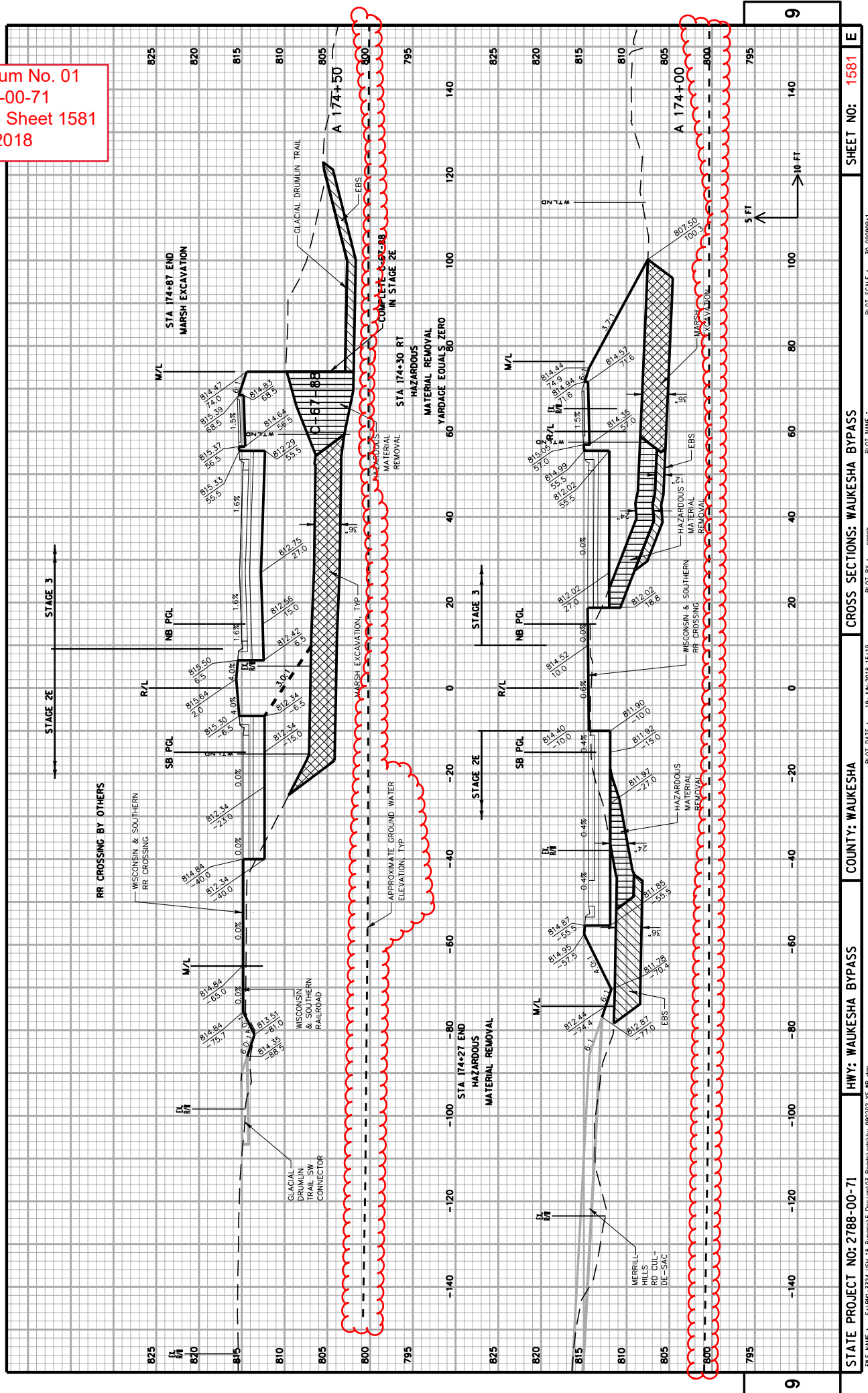
Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1580  
 July 2, 2018



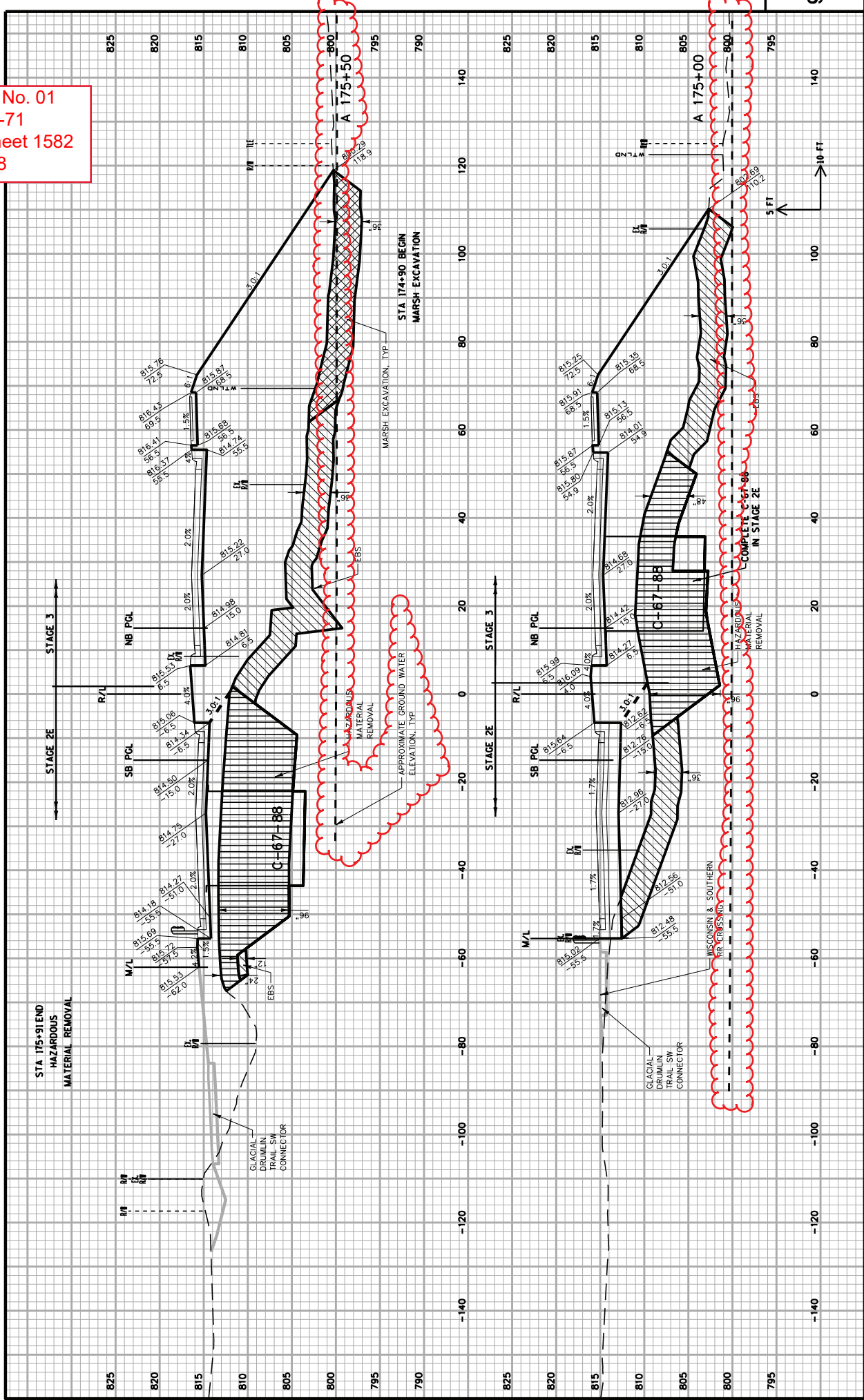
STATE PROJECT NO: 2788-00-71 COUNTY: WAUKESHA CROSS SECTIONS: WAUKESHA BYPASS SHEET NO: 1580 E

FILE NAME : F:\NM\3324 USH 18 Bypass\5-Design\03\_Roads\cvs by\_090202.XS.WB.dgn PLOT DATE : 19-JUN-2018 15:00 PLOT BY : scook PLOT SCALE : 30.090909:1

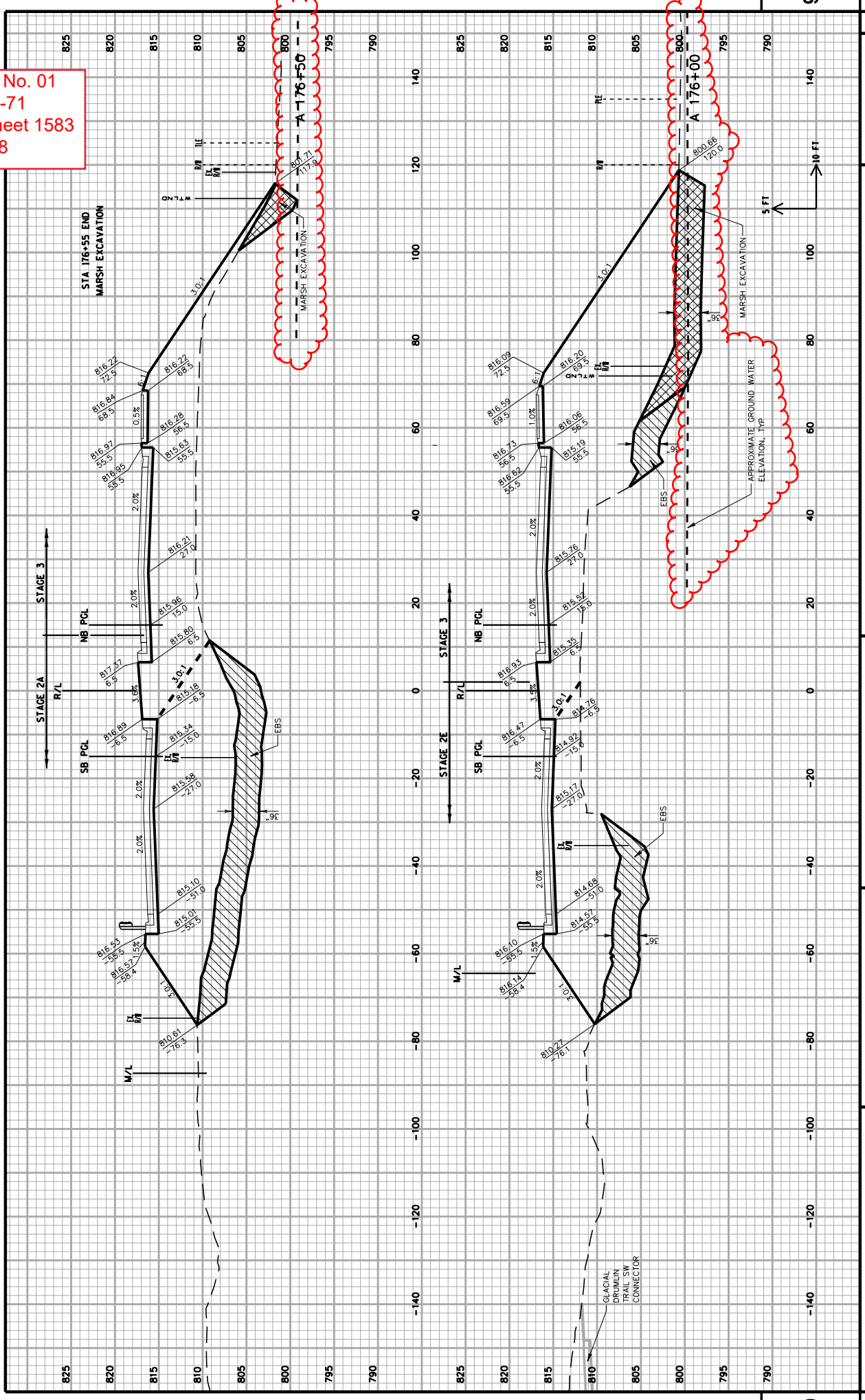
Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1581  
 July 2, 2018



Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1582  
 July 2, 2018



Addendum No. 01  
 ID 2788-00-71  
 Revised Sheet 1583  
 July 2, 2018





Proposal Schedule of Items

Proposal ID: 20180710010 Project(s): 2788-00-71

Federal ID(s): WISC 2017512

SECTION: 0001 Contract Items

Alt Set ID: Alt Mbr ID:

| Proposal Line Number | Item ID Description                                                                           | Approximate Quantity and Units | Unit Price | Bid Amount |
|----------------------|-----------------------------------------------------------------------------------------------|--------------------------------|------------|------------|
| 0002                 | 201.0105<br>Clearing                                                                          | 242.000<br>STA                 | _____.     | _____.     |
| 0004                 | 201.0205<br>Grubbing                                                                          | 242.000<br>STA                 | _____.     | _____.     |
| 0006                 | 203.0100<br>Removing Small Pipe Culverts                                                      | 17.000<br>EACH                 | _____.     | _____.     |
| 0008                 | 203.0200<br>Removing Old Structure (station) 01.<br>245+25                                    | LS                             | LUMP SUM   | _____.     |
| 0010                 | 203.0600.S<br>Removing Old Structure Over Waterway<br>With Minimal Debris (station) 01. 55+50 | LS                             | LUMP SUM   | _____.     |
| 0012                 | 204.0100<br>Removing Pavement                                                                 | 18,250.000<br>SY               | _____.     | _____.     |
| 0014                 | 204.0110<br>Removing Asphaltic Surface                                                        | 5,940.000<br>SY                | _____.     | _____.     |
| 0016                 | 204.0130<br>Removing Curb                                                                     | 385.000<br>LF                  | _____.     | _____.     |
| 0018                 | 204.0150<br>Removing Curb & Gutter                                                            | 8,440.000<br>LF                | _____.     | _____.     |
| 0020                 | 204.0155<br>Removing Concrete Sidewalk                                                        | 1,410.000<br>SY                | _____.     | _____.     |
| 0022                 | 204.0165<br>Removing Guardrail                                                                | 1,529.000<br>LF                | _____.     | _____.     |
| 0024                 | 204.0170<br>Removing Fence                                                                    | 390.000<br>LF                  | _____.     | _____.     |
| 0026                 | 204.0190<br>Removing Surface Drains                                                           | 1.000<br>EACH                  | _____.     | _____.     |
| 0028                 | 204.0195<br>Removing Concrete Bases                                                           | 16.000<br>EACH                 | _____.     | _____.     |
| 0030                 | 204.0210<br>Removing Manholes                                                                 | 2.000<br>EACH                  | _____.     | _____.     |
| 0032                 | 204.0220<br>Removing Inlets                                                                   | 34.000<br>EACH                 | _____.     | _____.     |



Proposal Schedule of Items

Proposal ID: 20180710010 Project(s): 2788-00-71

Federal ID(s): WISC 2017512

SECTION: 0001 Contract Items

Alt Set ID: Alt Mbr ID:

| Proposal Line Number | Item ID Description                                             | Approximate Quantity and Units | Unit Price | Bid Amount |
|----------------------|-----------------------------------------------------------------|--------------------------------|------------|------------|
| 0262                 | 601.0409<br>Concrete Curb & Gutter 30-Inch Type A               | 3,195.000<br>LF                | _____.     | _____.     |
| 0264                 | 601.0411<br>Concrete Curb & Gutter 30-Inch Type D               | 3,905.000<br>LF                | _____.     | _____.     |
| 0266                 | 601.0413<br>Concrete Curb & Gutter 6-Inch Sloped 30-Inch Type G | 38,680.000<br>LF               | _____.     | _____.     |
| 0268                 | 601.0415<br>Concrete Curb & Gutter 6-Inch Sloped 30-Inch Type J | 5,270.000<br>LF                | _____.     | _____.     |
| 0270                 | 601.0557<br>Concrete Curb & Gutter 6-Inch Sloped 36-Inch Type D | 250.000<br>LF                  | _____.     | _____.     |
| 0272                 | 601.0574<br>Concrete Curb & Gutter 4-Inch Sloped 30-Inch Type G | 2,050.000<br>LF                | _____.     | _____.     |
| 0274                 | 601.0576<br>Concrete Curb & Gutter 4-Inch Sloped 30-Inch Type J | 480.000<br>LF                  | _____.     | _____.     |
| 0276                 | 601.0600<br>Concrete Curb Pedestrian                            | 155.000<br>LF                  | _____.     | _____.     |
| 0278                 | 602.0410<br>Concrete Sidewalk 5-Inch                            | 105,800.000<br>SF              | _____.     | _____.     |
| 0280                 | 602.0420<br>Concrete Sidewalk 7-Inch                            | 105.000<br>SF                  | _____.     | _____.     |
| 0282                 | 602.0505<br>Curb Ramp Detectable Warning Field Yellow           | 1,175.000<br>SF                | _____.     | _____.     |
| 0284                 | 602.0605<br>Curb Ramp Detectable Warning Field Radial Yellow    | 190.200<br>SF                  | _____.     | _____.     |
| 0286                 | 603.1132<br>Concrete Barrier Type S32                           | 10.000<br>LF                   | _____.     | _____.     |
| 0288                 | 603.1136<br>Concrete Barrier Type S36                           | 42.000<br>LF                   | _____.     | _____.     |
| 0290                 | 603.1236<br>Concrete Barrier Type S36A                          | 2,583.000<br>LF                | _____.     | _____.     |



Proposal Schedule of Items

Proposal ID: 20180710010 Project(s): 2788-00-71

Federal ID(s): WISC 2017512

SECTION: 0001 Contract Items

Alt Set ID: Alt Mbr ID:

| Proposal Line Number | Item ID Description                                                                            | Approximate Quantity and Units | Unit Price | Bid Amount |
|----------------------|------------------------------------------------------------------------------------------------|--------------------------------|------------|------------|
| 0320                 | 608.0415<br>Storm Sewer Pipe Reinforced Concrete Class IV 15-Inch                              | 4,817.000<br>LF                | _____.     | _____.     |
| 0322                 | 608.0418<br>Storm Sewer Pipe Reinforced Concrete Class IV 18-Inch                              | 2,429.000<br>LF                | _____.     | _____.     |
| 0324                 | 608.0424<br>Storm Sewer Pipe Reinforced Concrete Class IV 24-Inch                              | 1,917.000<br>LF                | _____.     | _____.     |
| 0326                 | 608.0430<br>Storm Sewer Pipe Reinforced Concrete Class IV 30-Inch                              | 111.000<br>LF                  | _____.     | _____.     |
| 0328                 | 608.2314<br>Storm Sewer Pipe Reinforced Concrete Horizontal Elliptical Class HE-III 14x23-Inch | 27.000<br>LF                   | _____.     | _____.     |
| 0330                 | 608.2324<br>Storm Sewer Pipe Reinforced Concrete Horizontal Elliptical Class HE-III 24x38-Inch | 127.000<br>LF                  | _____.     | _____.     |
| 0332                 | 608.2329<br>Storm Sewer Pipe Reinforced Concrete Horizontal Elliptical Class HE-III 29x45-Inch | 36.000<br>LF                   | _____.     | _____.     |
| 0334                 | 608.2419<br>Storm Sewer Pipe Reinforced Concrete Horizontal Elliptical Class HE-IV 19x30-Inch  | 623.000<br>LF                  | _____.     | _____.     |
| 0336                 | 611.0430<br>Reconstructing Inlets                                                              | 3.000<br>EACH                  | _____.     | _____.     |
| 0338                 | 611.0530<br>Manhole Covers Type J                                                              | 18.000<br>EACH                 | _____.     | _____.     |
| 0340                 | 611.0535<br>Manhole Covers Type J-Special                                                      | 15.000<br>EACH                 | _____.     | _____.     |
| 0342                 | 611.0610<br>Inlet Covers Type BW                                                               | 5.000<br>EACH                  | _____.     | _____.     |
| 0344                 | 611.0624<br>Inlet Covers Type H                                                                | 26.000<br>EACH                 | _____.     | _____.     |



Proposal Schedule of Items

Proposal ID: 20180710010 Project(s): 2788-00-71

Federal ID(s): WISC 2017512

SECTION: 0001 Contract Items

Alt Set ID: Alt Mbr ID:

| Proposal Line Number | Item ID Description                    | Approximate Quantity and Units | Unit Price | Bid Amount |
|----------------------|----------------------------------------|--------------------------------|------------|------------|
| 0346                 | 611.0630<br>Inlet Covers Type HM-GJ    | 204.000<br>EACH                | _____.     | _____.     |
| 0348                 | 611.0633<br>Inlet Covers Type HM-GJ-S  | 17.000<br>EACH                 | _____.     | _____.     |
| 0350                 | 611.0639<br>Inlet Covers Type H-S      | 4.000<br>EACH                  | _____.     | _____.     |
| 0352                 | 611.0642<br>Inlet Covers Type MS       | 52.000<br>EACH                 | _____.     | _____.     |
| 0354                 | 611.0654<br>Inlet Covers Type V        | 25.000<br>EACH                 | _____.     | _____.     |
| 0356                 | 611.1004<br>Catch Basins 4-FT Diameter | 6.000<br>EACH                  | _____.     | _____.     |
| 0358                 | 611.1005<br>Catch Basins 5-FT Diameter | 1.000<br>EACH                  | _____.     | _____.     |
| 0360                 | 611.1006<br>Catch Basins 6-FT Diameter | 3.000<br>EACH                  | _____.     | _____.     |
| 0362                 | 611.1230<br>Catch Basins 2x3-FT        | 3.000<br>EACH                  | _____.     | _____.     |
| 0364                 | 611.2004<br>Manholes 4-FT Diameter     | 24.000<br>EACH                 | _____.     | _____.     |
| 0366                 | 611.2005<br>Manholes 5-FT Diameter     | 21.000<br>EACH                 | _____.     | _____.     |
| 0368                 | 611.2006<br>Manholes 6-FT Diameter     | 8.000<br>EACH                  | _____.     | _____.     |
| 0370                 | 611.2007<br>Manholes 7-FT Diameter     | 2.000<br>EACH                  | _____.     | _____.     |
| 0372                 | 611.3004<br>Inlets 4-FT Diameter       | 31.000<br>EACH                 | _____.     | _____.     |
| 0374                 | 611.3220<br>Inlets 2x2-FT              | 15.000<br>EACH                 | _____.     | _____.     |
| 0376                 | 611.3225<br>Inlets 2x2.5-FT            | 2.000<br>EACH                  | _____.     | _____.     |
| 0378                 | 611.3230<br>Inlets 2x3-FT              | 183.000<br>EACH                | _____.     | _____.     |





Proposal Schedule of Items

Proposal ID: 20180710010 Project(s): 2788-00-71

Federal ID(s): WISC 2017512

SECTION: 0001 Contract Items

Alt Set ID: Alt Mbr ID:

| Proposal Line Number | Item ID Description                                        | Approximate Quantity and Units | Unit Price | Bid Amount |
|----------------------|------------------------------------------------------------|--------------------------------|------------|------------|
| 0542                 | 638.3000<br>Removing Small Sign Supports                   | 309.000<br>EACH                | _____.     | _____.     |
| 0544                 | 638.3100<br>Removing Structural Steel Sign Supports        | 8.000<br>EACH                  | _____.     | _____.     |
| 0546                 | 638.3210<br>Revising Signs Type I Demountable              | 1.000<br>EACH                  | _____.     | _____.     |
| 0548                 | 640.1303.S<br>Pond Liner Clay                              | 772.000<br>CY                  | _____.     | _____.     |
| 0550                 | 641.8100<br>Overhead Sign Support (structure) 01. S-67-975 | LS                             | LUMP SUM   | _____.     |
| 0552                 | 643.0300<br>Traffic Control Drums                          | 201,000.000<br>DAY             | _____.     | _____.     |
| 0554                 | 643.0410<br>Traffic Control Barricades Type II             | 2,000.000<br>DAY               | _____.     | _____.     |
| 0556                 | 643.0420<br>Traffic Control Barricades Type III            | 37,000.000<br>DAY              | _____.     | _____.     |
| 0558                 | 643.0500<br>Traffic Control Flexible Tubular Marker Posts  | 1,360.000<br>EACH              | _____.     | _____.     |
| 0560                 | 643.0600<br>Traffic Control Flexible Tubular Marker Bases  | 1,360.000<br>EACH              | _____.     | _____.     |
| 0562                 | 643.0705<br>Traffic Control Warning Lights Type A          | 59,500.000<br>DAY              | _____.     | _____.     |
| 0564                 | 643.0715<br>Traffic Control Warning Lights Type C          | 31,900.000<br>DAY              | _____.     | _____.     |
| 0566                 | 643.0800<br>Traffic Control Arrow Boards                   | 1,380.000<br>DAY               | _____.     | _____.     |
| 0568                 | 643.0900<br>Traffic Control Signs                          | 176,000.000<br>DAY             | _____.     | _____.     |
| 0570                 | 643.0920<br>Traffic Control Covering Signs Type II         | 30.000<br>EACH                 | _____.     | _____.     |
| 0572                 | 643.1000<br>Traffic Control Signs Fixed Message            | 1,450.000<br>SF                | _____.     | _____.     |



Proposal Schedule of Items

Proposal ID: 20180710010 Project(s): 2788-00-71

Federal ID(s): WISC 2017512

SECTION: 0001 Contract Items

Alt Set ID: Alt Mbr ID:

| Proposal Line Number | Item ID Description                                                                         | Approximate Quantity and Units | Unit Price | Bid Amount |
|----------------------|---------------------------------------------------------------------------------------------|--------------------------------|------------|------------|
| 0664                 | 650.6500<br>Construction Staking Structure Layout (structure) 08. B-67-355                  | LS                             | LUMP SUM   | _____.     |
| 0666                 | 650.6500<br>Construction Staking Structure Layout (structure) 09. C-67-88                   | LS                             | LUMP SUM   | _____.     |
| 0668                 | 650.6500<br>Construction Staking Structure Layout (structure) 10. C-67-91                   | LS                             | LUMP SUM   | _____.     |
| 0670                 | 650.6500<br>Construction Staking Structure Layout (structure) 11. R-67-129                  | LS                             | LUMP SUM   | _____.     |
| 0672                 | 650.6500<br>Construction Staking Structure Layout (structure) 12. R-67-145                  | LS                             | LUMP SUM   | _____.     |
| 0674                 | 650.6500<br>Construction Staking Structure Layout (structure) 13. S-67-975                  | LS                             | LUMP SUM   | _____.     |
| 0676                 | 650.7000<br>Construction Staking Concrete Pavement                                          | 17,960.000<br>LF               | _____.     | _____.     |
| 0678                 | 650.8500<br>Construction Staking Electrical Installations (project) 01. 2788-00-71 Signals  | LS                             | LUMP SUM   | _____.     |
| 0680                 | 650.8500<br>Construction Staking Electrical Installations (project) 02. 2788-00-71 Lighting | LS                             | LUMP SUM   | _____.     |
| 0682                 | 650.9000<br>Construction Staking Curb Ramps                                                 | 112.000<br>EACH                | _____.     | _____.     |
| 0684                 | 650.9910<br>Construction Staking Supplemental Control (project) 01. 2788-00-71              | LS                             | LUMP SUM   | _____.     |
| 0686                 | 650.9920<br>Construction Staking Slope Stakes                                               | 21,860.000<br>LF               | _____.     | _____.     |
| 0688                 | 652.0225<br>Conduit Rigid Nonmetallic Schedule 40 2-Inch                                    | 8,674.000<br>LF                | _____.     | _____.     |



Proposal Schedule of Items

Proposal ID: 20180710010 Project(s): 2788-00-71

Federal ID(s): WISC 2017512

SECTION: 0001 Contract Items

Alt Set ID: Alt Mbr ID:

| Proposal Line Number | Item ID Description                                                      | Approximate Quantity and Units | Unit Price | Bid Amount |
|----------------------|--------------------------------------------------------------------------|--------------------------------|------------|------------|
| 0924                 | SPV.0090<br>Special 02. FENCE CHAIN LINK<br>POLYMER COATED 4-FT          | 359.000<br>LF                  | _____.     | _____.     |
| 0926                 | SPV.0090<br>Special 03. FENCE CHAIN LINK<br>POLYMER COATED 6-FT          | 808.000<br>LF                  | _____.     | _____.     |
| 0928                 | SPV.0090<br>Special 04. CONSTRUCTION STAKING<br>SIDEWALK                 | 21,200.000<br>LF               | _____.     | _____.     |
| 0930                 | SPV.0090<br>Special 05. CONSTRUCTION STAKING<br>FENCE SAFETY             | 1,855.000<br>LF                | _____.     | _____.     |
| 0932                 | SPV.0090<br>Special 41. TYPE UF CABLE, 2<br>CONDUCTOR, NO. 14            | 6,098.000<br>LF                | _____.     | _____.     |
| 0934                 | SPV.0090<br>Special 71. PRE-PAVING TELEVISION<br>SANITARY MAIN LINE      | 288.000<br>LF                  | _____.     | _____.     |
| 0936                 | SPV.0090<br>Special 72. WATER MAIN RELOCATION<br>8-INCH                  | 45.000<br>LF                   | _____.     | _____.     |
| 0938                 | SPV.0090<br>Special 73. WATER MAIN RELOCATION<br>12-INCH                 | 45.000<br>LF                   | _____.     | _____.     |
| 0940                 | SPV.0090<br>Special 74. WATER MAIN RELOCATION<br>16-INCH                 | 93.000<br>LF                   | _____.     | _____.     |
| 0942                 | SPV.0090<br>Special 75. WATER MAIN RELOCATION<br>20-INCH                 | 78.000<br>LF                   | _____.     | _____.     |
| 0944                 | SPV.0090<br>Special 76. CONDUIT FLEXIBLE<br>METALLIC 2-INCH              | 15.000<br>LF                   | _____.     | _____.     |
| 0946                 | SPV.0090<br>Special 77. 3/4-INCH LIQUID TIGHT<br>CONDUIT                 | 10.000<br>LF                   | _____.     | _____.     |
| 0948                 | SPV.0090<br>Special 78. CONDUIT REINFORCED<br>THERMOSETTING RESIN 1-INCH | 130.000<br>LF                  | _____.     | _____.     |



Proposal Schedule of Items

Proposal ID: 20180710010 Project(s): 2788-00-71

Federal ID(s): WISC 2017512

SECTION: 0001 Contract Items

Alt Set ID: Alt Mbr ID:

| Proposal Line Number | Item ID Description                                                        | Approximate Quantity and Units | Unit Price        | Bid Amount |
|----------------------|----------------------------------------------------------------------------|--------------------------------|-------------------|------------|
| 1022                 | 205.0200<br>Excavation Rock                                                | 110.000<br>CY                  | _____.            | _____.     |
| 1024                 | 608.0005<br>Storm Sewer Rock Excavation                                    | 120.000<br>CY                  | _____.            | _____.     |
| 1026                 | 652.0125<br>Conduit Rigid Metallic 2-Inch                                  | 60.000<br>LF                   | _____.            | _____.     |
| 1028                 | 653.0220<br>Junction Boxes 18x6x6-Inch                                     | 6.000<br>EACH                  | _____.            | _____.     |
| 1030                 | 999.1000.S<br>Seismograph                                                  | LS                             | LUMP SUM          | _____.     |
| 1032                 | SPV.0090<br>Special 06. Wall R-67-145 Fence Chain Link Polymer Coated 4-Ft | 228.000<br>LF                  | _____.            | _____.     |
| 1034                 | SPV.0090<br>Special 07. Fence Chain Link Polymer Coated 4-Ft               | 1,092.000<br>LF                | _____.            | _____.     |
| 1036                 | SPV.0090<br>Special 08. Temporary Precast Trench Drain                     | 230.000<br>LF                  | _____.            | _____.     |
| <b>Section: 0001</b> |                                                                            |                                | <b>Total:</b>     | _____.     |
|                      |                                                                            |                                | <b>Total Bid:</b> | _____.     |