

ORDER OF SHEETS

Section No.	Title
1	1
2	Typical Sections and Details (Includes Erosion Control Plan)
3	Estimate of Quantities
3	Miscellaneous Quantities
4	Right of Way Plat
5	Plan and Profile
6	Standard Detail Drawings
7	Sign Plates
8	Structure Plans
9	Computer Earthwork Data
9	Cross Sections

TOTAL SHEETS = 442



05

DESIGN DESIGNATION

A.A.D.T. (2022)	=	880
A.A.D.T. (2042)	=	1310
D.H.V. (2042)	=	118
D.D.	=	60/40
T.	=	6.9%
DESIGN SPEED	=	30 MPH (STA. 10+24 - STA. 32+00) 40 MPH (STA. 32+00 - STA. 265+24.10)
ESALS	=	1,60,000

CONVENTIONAL SYMBOLS

PLAN	PROFILE
CORPORATE LIMITS	GRADE LINE
PROPERTY LINE	ORIGINAL GROUND
LOT LINE	MARSH OR ROCK PROFILE (To be noted as such)
LIMITED HIGHWAY EASEMENT	SPECIAL DITCH
EXISTING RIGHT OF WAY	GRADE ELEVATION
PROPOSED OR NEW R/W LINE	CULVERT (Profile View)
SLOPE INTERCEPT	UTILITIES
REFERENCE LINE	ELECTRIC
EXISTING CULVERT	FIBER OPTIC
PROPOSED CULVERT (Box or Pipe)	GAS
COMBUSTIBLE FLUIDS	SANITARY SEWER
	STORM SEWER
	TELEPHONE
MARSH AREA	WATER
	UTILITY PEDESTAL
	POWER POLE
WOODED OR SHRUB AREA	TELEPHONE POLE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

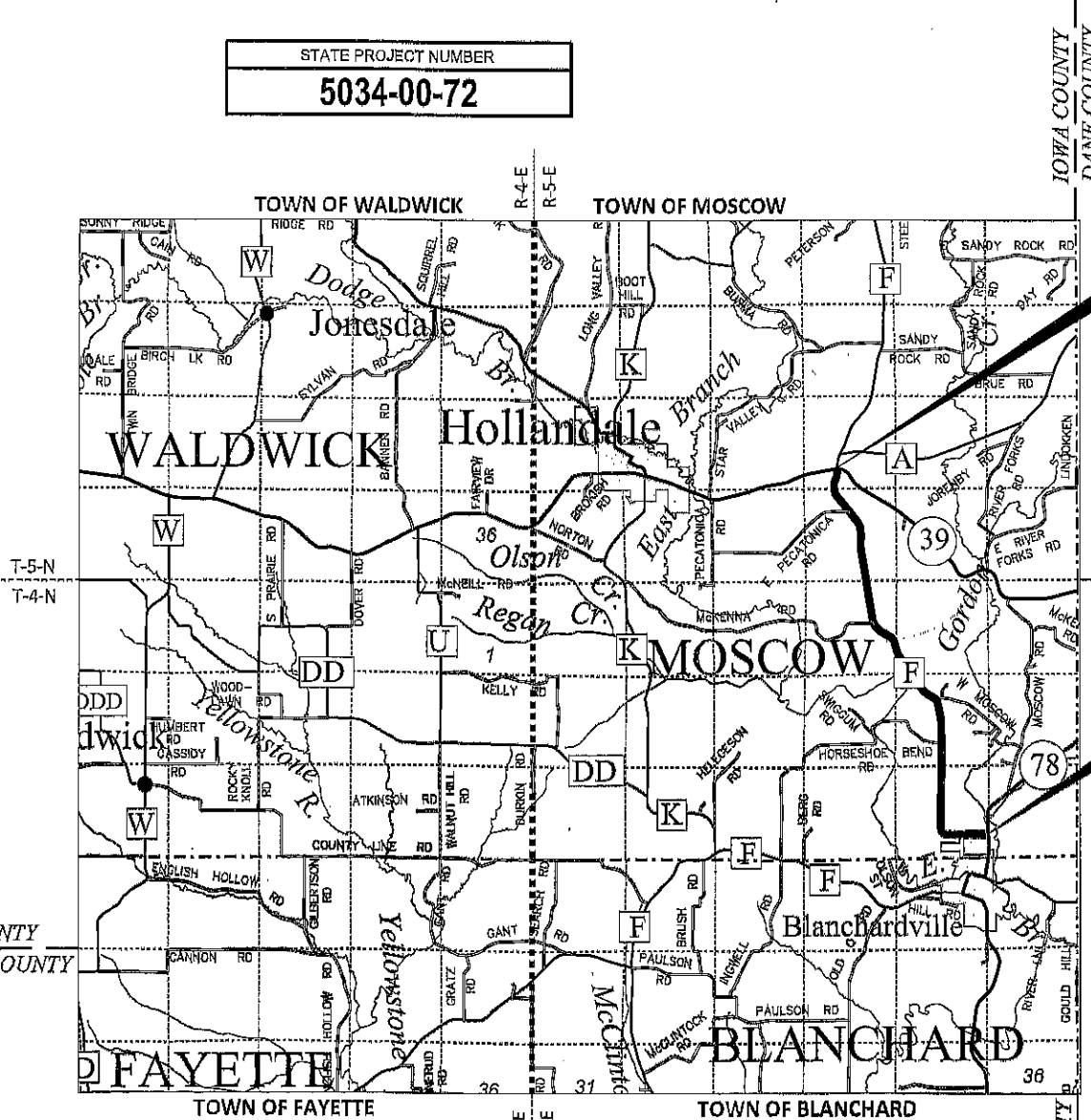
BLANCHARDVILLE - DANE COUNTY LINE

(STH 78 TO STH 39)

CTH F

IOWA COUNTY

STATE PROJECT NUMBER
5034-00-72



END PROJECT
STA. 265+24.10

BEGIN PROJECT
STA. 10+24
Y = 101,873.38
X = 452,385.85

LAYOUT
SCALE 0 2 MI
TOTAL NET LENGTH OF CENTERLINE = 4.830 MI

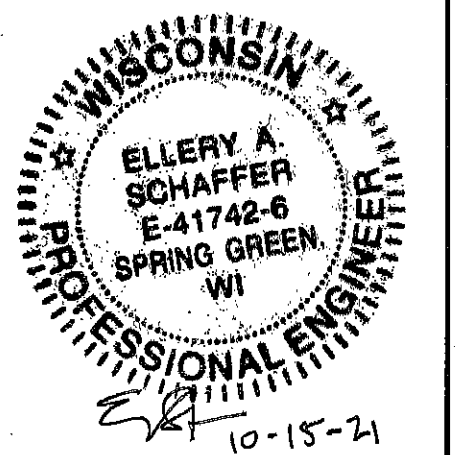
HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COUNTY COORDINATES, IOWA COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCE MAY BE USED AS GROUND DISTANCES.
ELEVATION SHOWN ON THIS PLAN ARE REFERENCE TO THE NORTH AMERICAN VERTICAL DATUM OF 1988, NAVD (2012).

DANE COUNTY
GREEN COUNTY

IOWA COUNTY
DANE COUNTY
GREEN COUNTY
LAFAYETTE COUNTY

ACCEPTED FOR
COUNTY of IOWA
Walter C. ...
(Date) (Highway Commissioner)

ORIGINAL PLANS PREPARED BY
JEWELL
associates engineers, inc
Engineers - Architects - Surveyors



STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY
Surveyor: JEWELL ASSOCIATES ENGINEERS, INC./
KD ENGINEERING CONSULTANTS
Designer: JEWELL ASSOCIATES ENGINEERS, INC.
Project Manager: ALEIGHA BURG, P.E.
Regional Examiner: SW REGION
Regional Supervisor: JOHN STOLZMAN, P.E.

APPROVED FOR THE DEPARTMENT
DATE: 10/19/2021
Aleigha Burg, P.E.
(Signature)

E

GENERAL NOTES

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLAN ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

NO TREES OR SHRUBS ARE TO BE REMOVED UNLESS SUCH TREES OR SHRUBS HAVE FIRST BEEN INDICATED FOR REMOVAL BY THE ENGINEER IN THE FIELD.

EXCAVATION BELOW SUBGRADE (EBS) IS NOT USED TO BALANCE YARDAGE, AND IS NOT SHOWN ON THE CROSS SECTIONS BUT IS MEASURED AND PAID FOR AS COMMON EXCAVATION. EXACT LOCATIONS OF EBS WILL BE DETERMINED BY THE ENGINEER.

DISTURBED AREAS SHOWN WITHIN THE RIGHT-OF-WAY, EXCEPT THE AREAS WITHIN THE FINISHED SHOULDER POINTS ARE TO BE FERTILIZED (TYPE B), SEEDED (USE SEED MIX NO. 20 OR SEED MIX NO. 40) AND MULCHED/EROSION MATTED AS DIRECTED BY THE ENGINEER. MULCH/EROSION MAT ALL MAINLINE SLOPES AS DIRECTED BY ENGINEER IN FIELD. ALL POST CONSTRUCTION WET AREAS SHALL BE SEEDED WITH SEEDING MIX NO. 60. DO NOT FERTILIZE WET AREAS.

WHEN THE QUANTITY OF THE ITEM OF BREAKER RUN, SELECT CRUSHED MATERIAL, BASE AGGREGATE DENSE OR HMA PAVEMENT IS MEASURED FOR PAYMENT BY THE TON, THE DEPTH OR THICKNESS OF THE LAYER SHOWN ON THE PLANS IS APPROXIMATE, AND THE ACTUAL THICKNESS WILL DEPEND ON THE DISTRIBUTION OF THE MATERIAL AS DIRECTED BY THE ENGINEER IN THE FIELD.

SILT FENCE, TEMPORARY DITCH CHECKS, TEMPORARY STONE DITCH CHECKS, AND CULVERT PIPE CHECK BID ITEMS SHALL BE PLACED AS SHOWN ON THE PLAN OR AS DIRECTED BY THE ENGINEER IN THE FIELD. SILT FENCE SHALL BE PLACED PRIOR TO CONSTRUCTION.

FILL EXPANSION IS VARIABLE AND IS ESTIMATED AT 25%.

THE EXACT LOCATION OF PRIVATE, FIELD, AND COMMERCIAL ENTRANCES TO BE DETERMINED BY THE ENGINEER IN THE FIELD.

REMOVAL OF ASPHALTIC/CONCRETE SURFACES WHERE AN ABUTTING ASPHALTIC/CONCRETE SURFACE IS TO REMAIN IN PLACE SHALL REQUIRE A SAWCUT MEETING THE APPROVAL OF THE ENGINEER IN THE FIELD.

INLET AND OUTLET ELEVATIONS FOR CULVERT PIPES AS SHOWN ON PLANS MAY BE ADJUSTED TO FIT FIELD CONDITIONS. ACCURACY OF INLET AND DISCHARGE ELEVATIONS FOR DRAINAGE STRUCTURES SHALL BE VERIFIED BY THE ENGINEER IN THE FIELD.

CURB AND GUTTER ELEVATIONS ARE GIVEN ON THE FLANGE LINE, UNLESS OTHERWISE NOTED.

ALL RADII DIMENSIONS ON THE PLAN ARE GIVEN TO THE EDGE OF ASPHALT.

EXISTING DRIVEWAYS SHALL BE RESTORED IN KIND AND THEIR LOCATION VERIFIED BY THE ENGINEER IN THE FIELD.

4.25-INCHES OF HMA PAVEMENT SHALL BE CONSTRUCTED WITH A 1.75-INCH UPPER LAYER (HMA PAVEMENT 4MT58-28S) AND A 2.5-INCH LOWER LAYER. HMA PAVEMENT QUANTITIES WERE CALCULATED USING 112 LB/SY/IN.

EXPANSION JOINTS SHALL BE CONSTRUCTED AT ALL RADII POINTS IN THE CURB & GUTTER.

STORM SEWER ELEVATIONS, LENGTHS, AND LOCATIONS AS SHOWN ON THE PLANS AND CROSS SECTIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS. ACCURACY OF INLET AND DISCHARGE ELEVATIONS FOR DRAINAGE STRUCTURES SHALL BE VERIFIED BY THE ENGINEER IN THE FIELD.

MISCELLANEOUS REMOVAL ITEMS REQUIRING RESTORATIONS OF CONCRETE OR ASPHALT DRIVEWAYS, SIDEWALKS, OR SIDE STREETS SHALL BE REMOVED TO AN EXISTING JOINT OR SAWED AS DETERMINED BY THE ENGINEER IN THE FIELD OR AS SHOWN ON THE PLANS.

WETLANDS ARE PRESENT WITHIN THE PROJECT LIMITS. THE CONTRACTOR SHALL NOT OPERATE EQUIPMENT BEYOND THE SLOPE INTERCEPTS FROM STA. 36+65 - STA. 43+02, RT., STA. 40+21 - STA 43+83, LT., AND STA. 50+61 - STA. 50+86, LT.

ADJUST DITCH GRADING AS NECESSARY TO FIELD CONDITIONS AND AS DIRECTED BY THE ENGINEER IN THE FIELD.

CURVE DATA IS BASED ON THE ARC DEFINITION.

THE LOCATION OF ALL PERMANENT SIGNING SHALL BE VERIFIED BY THE ENGINEER IN THE FIELD PRIOR TO PLACEMENT.

INSTALL ADDITIONAL EXPANSION JOINTS AS DIRECTED BY THE ENGINEER IN THE FIELD.

TRANSVERSE JOINTS IN CONCRETE SIDEWALK SHALL BE CONSTRUCTED AT INTERVALS EQUAL TO THE WIDTH OF THE CONCRETE SIDEWALK, UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

THE CONTRACTOR'S PAVING OPERATIONS SHALL BE CONSISTENT WITH THE PLAN TYPICAL SECTIONS AND CONSTRUCTED TO PREVENT HMA LONGITUDINAL JOINTS FROM BEING LOCATED WITHIN A DRIVING , TURNING, BIKE, OR PASSING LANE.

CONTACTS

DESIGN CONSULTANT
JEWELL ASSOCIATES ENGINEERS, INC.
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CELL: (608) 341-8159
EMAIL: ellery.schaffer@jewellassoc.com

WDNR LIAISON
STATE OF WISCONSIN
DNR SOUTH CENTRAL REGION HQ
3911 FISH HATCHERY ROAD
FITCHBURG, WI 53711
ATTN: ANDY BARTA
PH: (608) 275-3308
CELL: (608) 235-2955
EMAIL: Andrew.Barta@wisconsin.gov

IOWA COUNTY HIGHWAY DEPARTMENT
CRAIG HARDY, COMMISSIONER
1215 NORTH BEQUETTE STREET
DODGEVILLE, WI 53533
PH: (608) 935-3381 EXT. 605
CELL: (608) 574-2935
EMAIL: craig.hardy@iowacounty.org

VILLAGE OF BLANCHARDVILLE
NICK CROOKS, PRESIDENT
603 S. MAIN ST.
P.O. BOX 338
BARNEVELD, WI 53507
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EMAIL: blanchvilclerk@tds.net

TOWN OF MOSCOW
JOE HENDRICKSON
8023 E PECATONICA ROAD
HOLLANDALE, WI 53544
PH: (608) 967-2560
CELL: (608) 482-3884
EMAIL: peg8@mhct.net

UTILITIES

ELECTRICITY
ALLIANT ENERGY
ATTN: JAY RUNDE
490 SHAKERAG STREET
MINERAL POINT, WI 53565
OFFICE: (608) 458-4224
CELL: (608) 341-0594
EMAIL: jayrunde@alliantenergy.com

COMMUNICATION LINE
FRONTIER COMMUNICATIONS
ATTN: JERRY MOORE
2222 WEST WISCONSIN STREET
PORTAGE, WI 53901
OFFICE: (608) 742-9507
EMAIL: jerald.r.moore@ftr.com

AMERICAN TRANSMISSION COMPANY
ATTN: RYAN STREHLOW, ATC CONSTRUCTION MANAGER
(CONTACT FOR GUY WIRE & ANCHOR REPLACEMENT)
PO BOX 47
WAUKESHA, WI 53187
CELL: (608) 215-7800
EMAIL: rstrehlow@atcllc.com

TDS TELECOM
ATTN: JERRY MYERS
525 JUNCTION ROAD
MADISON, WI 53717
OFFICE: (608) 664-4404
EMAIL: jerry.myers@tdstelecom.com

ATTN: DOUG VOSBURG, ATC MAINTENANCE ENGINEER
(CONTACT IF GROUND ROD/WIRE IS DISTURBED, EXPOSED OR DAMAGED)
PO BOX 47
WAUKESHA, WI 53187
OFFICE: (608) 436-7650 OR (608) 877-7650

GAS/PETROLEUM
ALLIANT ENERGY
ATTN: JAY RUNDE
490 SHAKERAG STREET
MINERAL POINT, WI 53565
OFFICE: (608) 458-4224
CELL: (608) 341-0594
EMAIL: jayrunde@alliantenergy.com

ORDER OF SECTION 2 SHEETS:

- WRITTEN MATERIAL
- PROJECT OVERVIEW
- TYPICAL SECTIONS
- CONSTRUCTION DETAILS
- INTERSECTION DETAILS
- PAVING DETAILS
- EROSION CONTROL DETAILS
- STORM SEWER DETAILS
- PERMANENT SIGNING/PAVEMENT MARKING PLAN
- TRAFFIC CONTROL DETAILS
- ALIGNMENT LAYOUT & TIES



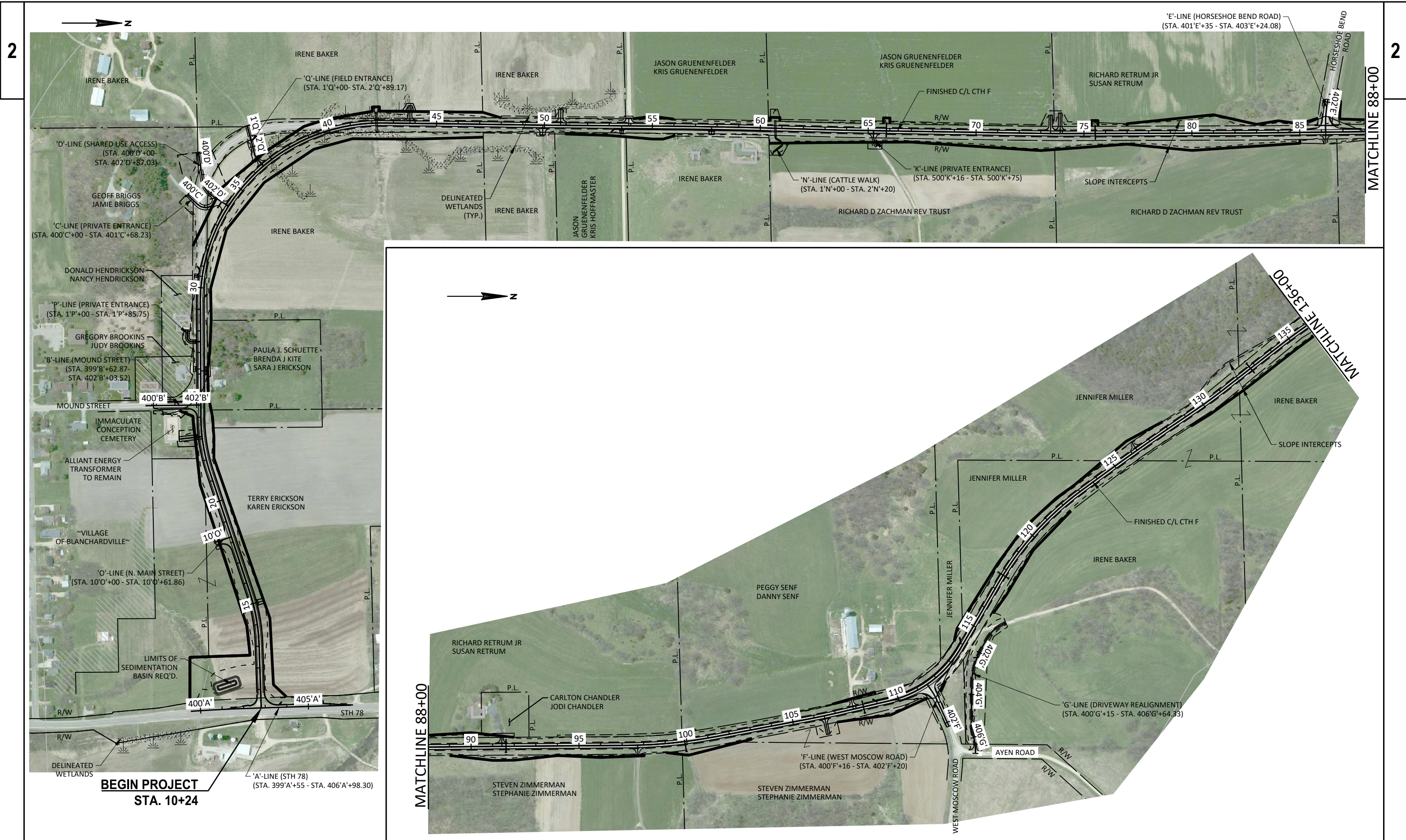
* DENOTES UTILITY IS NOT A MEMBER OF DIGGERS HOTLINE

LIST OF STANDARD ABBREVIATIONS

Table with 6 columns: Abbreviation, Description, Abbreviation, Description, Abbreviation, Description. Includes terms like ABUT, AC, AGG, AH, <, ASPH, AVG, ADT, BAD, BK, BF, BM, BR, C or C/L, CC, C.E., CTH, CR, CR, CY or CU YD, CP, C & G, D, DHV, DIA, E, X, ELEC, EL or ELEV, ESALS, EBS, FF, F.E., F, FG, FL or F/L, FT, FTG, GN, HT, CWT, HYD, INL, ID, INV, IP, IRS, JT, JCT, LHF, L, LIN FT or LF, LC, MH, MB, ML or M/L, N, Y, OD, PLE, PT, PC, PI, PRC, PT, POC, POT, PVC, PCC, LB, PSI, P.E., R, RR, R, RL or R/L, RP, RCCP, REQD, RES, RW, RT, RHF, R/W, RD, R, RDWY, SALV, SAN S, SEC, SHLDR, SHR, SW, S, SQ, SF or SQ FT, SY or SQ YD, STD, SDD, STH, STA, SS, SG, SE, SL or S/L, SV, T, TEL, TEMP, TI, TLE, t, T or TN, TRANS, TL or T/L, T, TYP, UNCL, UG, USH, VAR, V, VERT, VC, VOL, WM, WV, W, WB, YD, Roadway Salvaged, Sanitary Sewer, Section, Shoulder, Shrinkage, Sidewalk, South, Square, Square Feet, Square Yard, Standard, Standard Detail Drawings, State Trunk Highways, Station, Storm Sewer, Subgrade, Superelevation, Survey Line, Septic Vent, Tangent, Telephone, Temporary, Temporary Interest, Temporary Limited Easement, Ton, Town, Transition, Transit Line, Trucks (percent of), Typical, Unclassified, Underground Cable, United States Highway, Variable, Velocity or Design Speed, Vertical, Vertical Curve, Volume, Water Main, Water Valve, West, Westbound, Yard.

Table with 5 columns: HYDROLOGIC SOIL GROUP (A, B, C, D), SLOPE RANGE (PERCENT). Rows include LAND USE, ROW CROPS, MEDIAN STRIP-TURF, SIDE SLOPE-TURF, PAVEMENT (ASPHALT, CONCRETE, BRICK, DRIVES, WALKS, ROOFS, GRAVEL ROADS, SHOULDERS).

TOTAL PROJECT AREA = 73.0 ACRES
TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = 59.0 ACRES



PROJECT NO: 5034-00-72

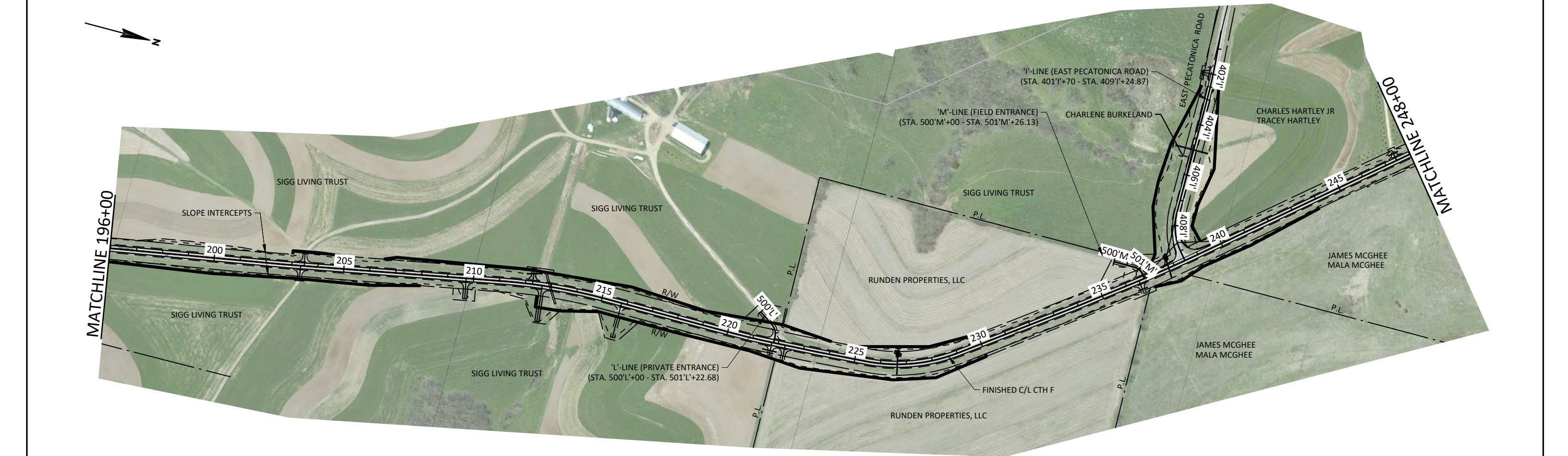
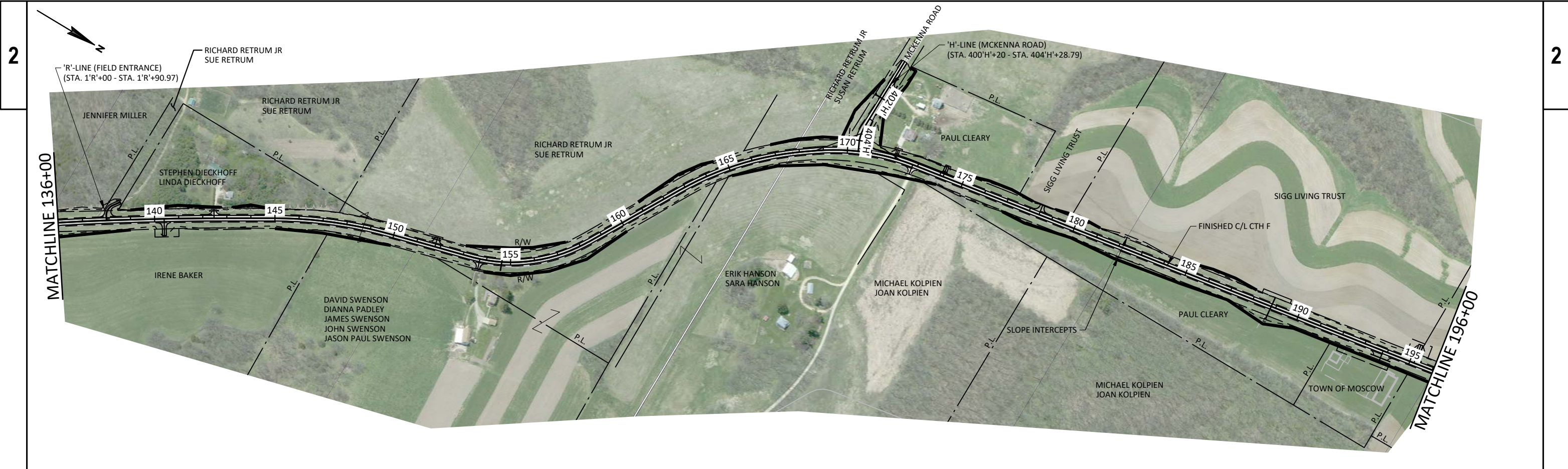
HWY: CTH F

COUNTY: IOWA

PROJECT OVERVIEW

SHEET

E



PROJECT NO: 5034-00-72

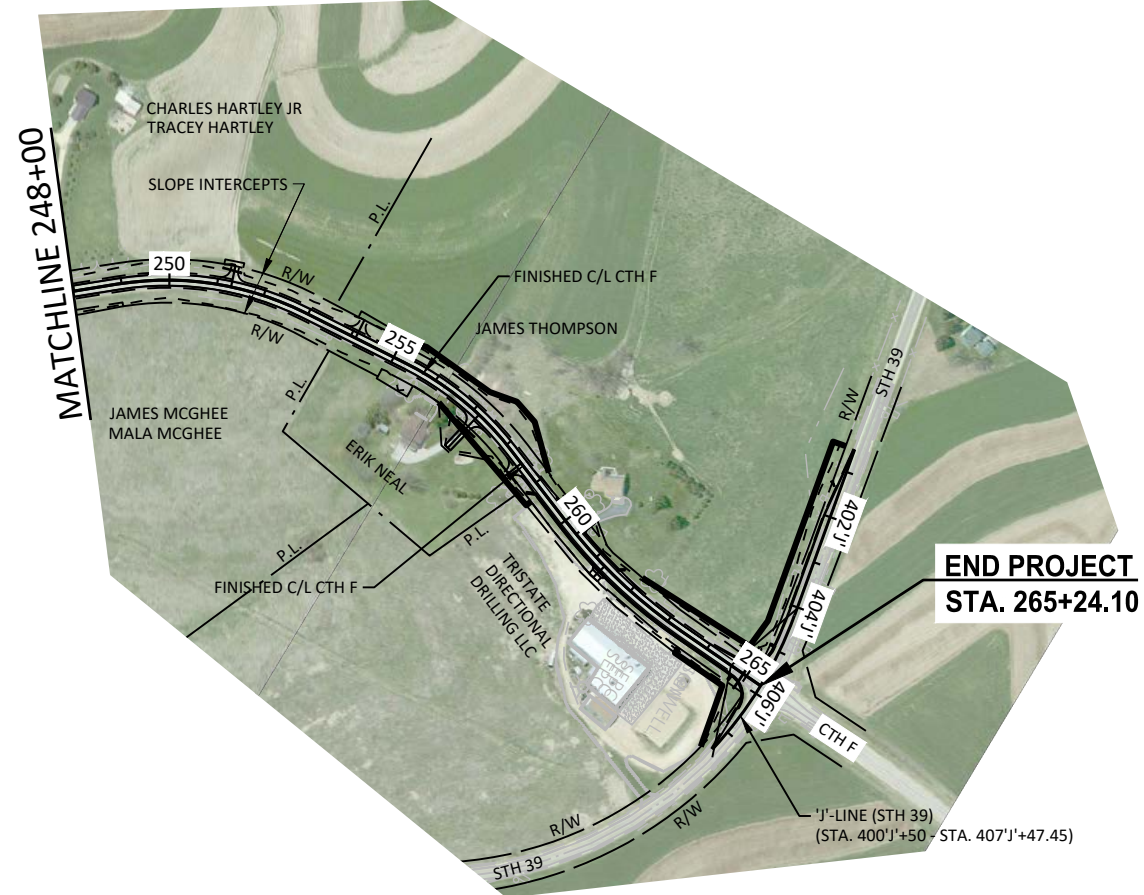
HWY: CTH F

COUNTY: IOWA

PROJECT OVERVIEW

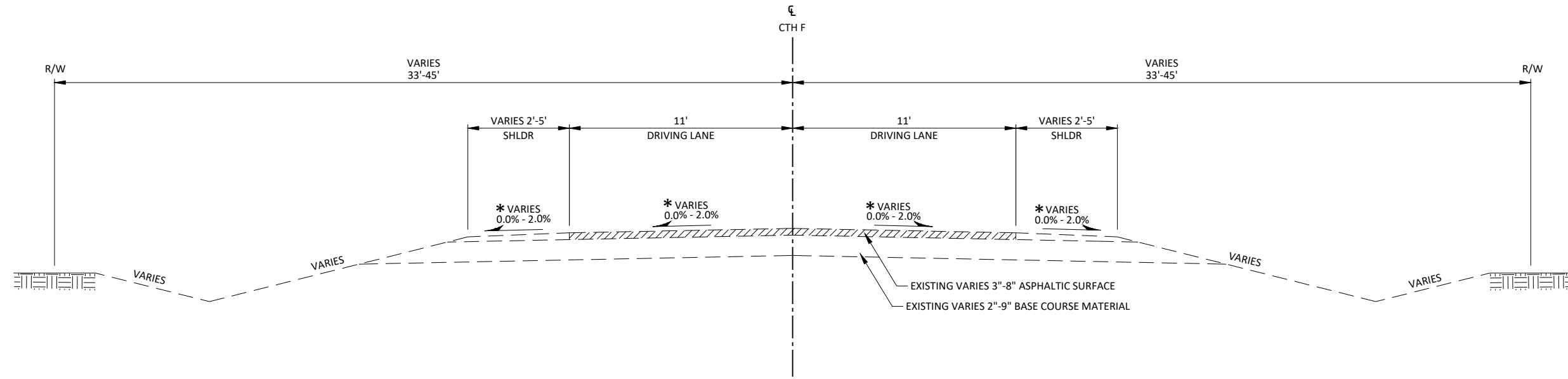
SHEET

E



FOR INFORMATION ONLY
BORING LOG

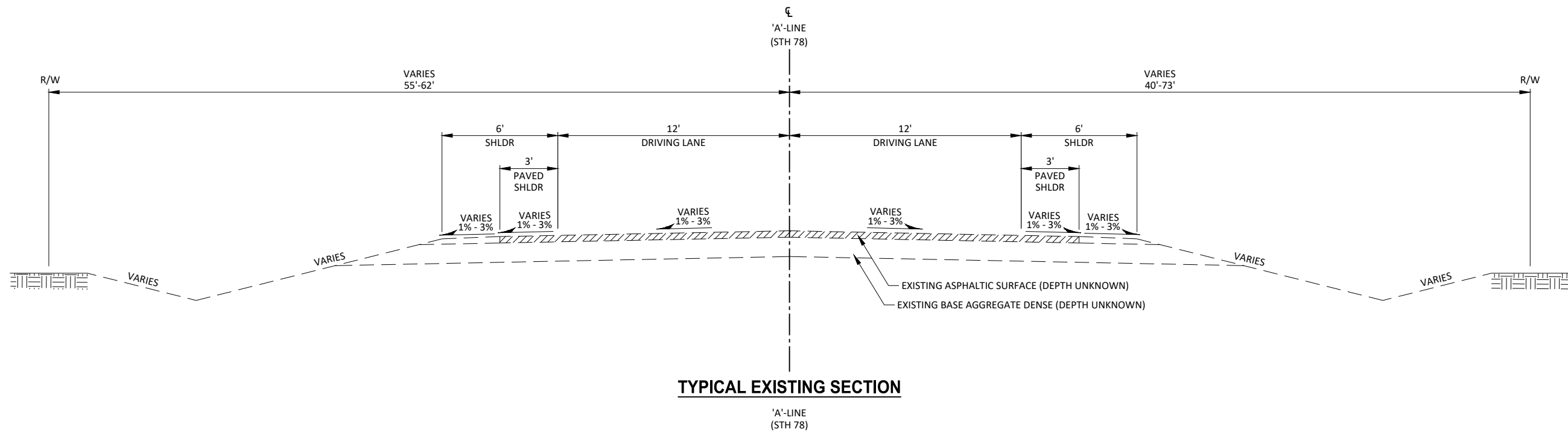
NO.	STATION/OFFSET	EXISTING ASPHALTIC SURFACE THICKNESS (IN.)	EXISTING B. A. D. THICKNESS (IN.)	LIMITS OF DOLOMITE BEDROCK ENCOUNTERED (DEPTH BELOW EXIST. SURFACE (FT.))
1	12+00, 0' RT.	-	-	-
2	21+50, 0' RT.	-	-	6.5
3	27+00, 6' RT.	5.5	4	2.5
4	38+00, 0' RT.	-	-	-
5	73+00, 10' RT.	3.5	4.5	-
6	86+00, 6' LT.	4	6	7.5
7	97+50, 15' LT.	-	-	7.5
8	111+25, 15' LT.	-	-	7.5
9	126+50, 4' LT.	4	6	3.5
10	139+00, 6' LT.	4	7	8
11	151+50, 2' RT.	3	5	-
12	172+00, 3' LT.	4	9	7
13	184+50, 5' RT.	4	7	-
14	193+50, 8' RT.	4	7	8.5
15	203+25, 10' RT.	3	4	5
16	221+70, 11' LT.	3.5	7	3.5
17	233+25, 2' LT.	5.5	4.5	7.5
18	252+50, 7' RT.	8	2	-
19	261+00, 3' LT.	5.5	7	-
20	265+00, 5' LT.	6	7	-
21	401'+75, 0' RT.	1	11	8



TYPICAL EXISTING SECTION

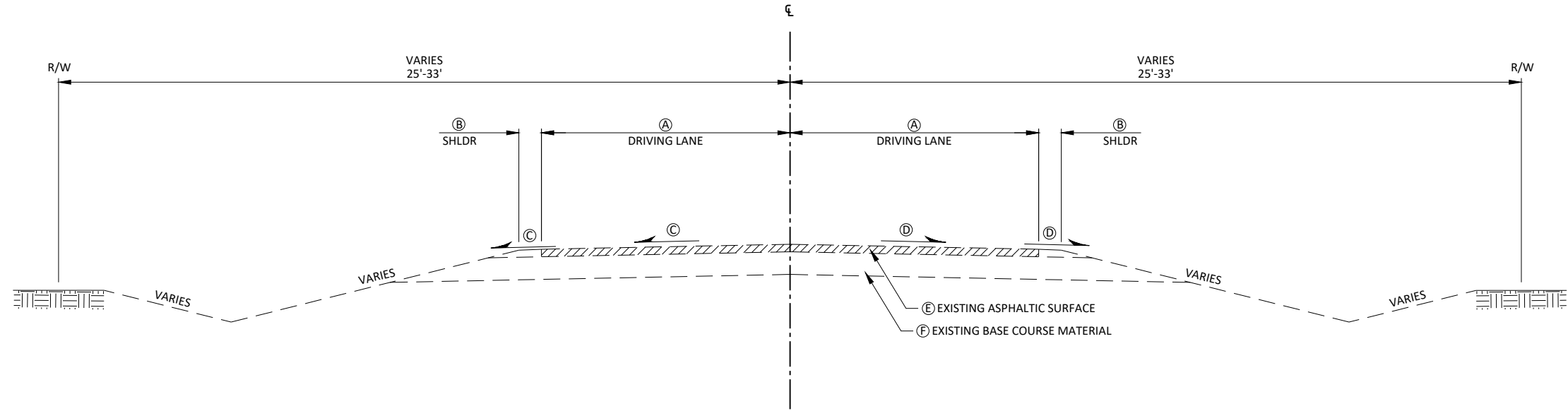
CTH F

* SUPERELEVATION RATES VARY
2.1%-12.1%



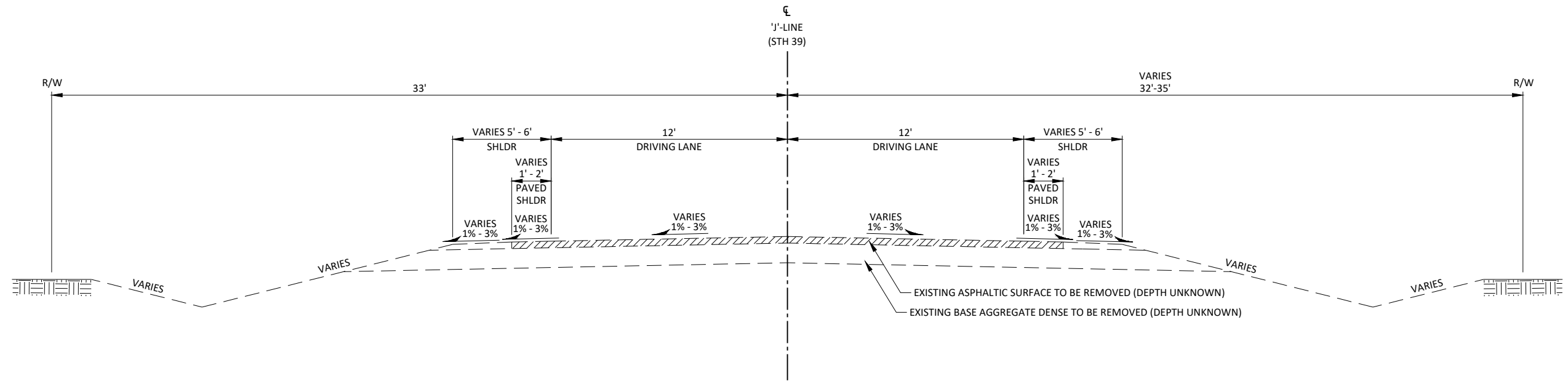
TYPICAL EXISTING SECTION

'A'-LINE
(STH 78)



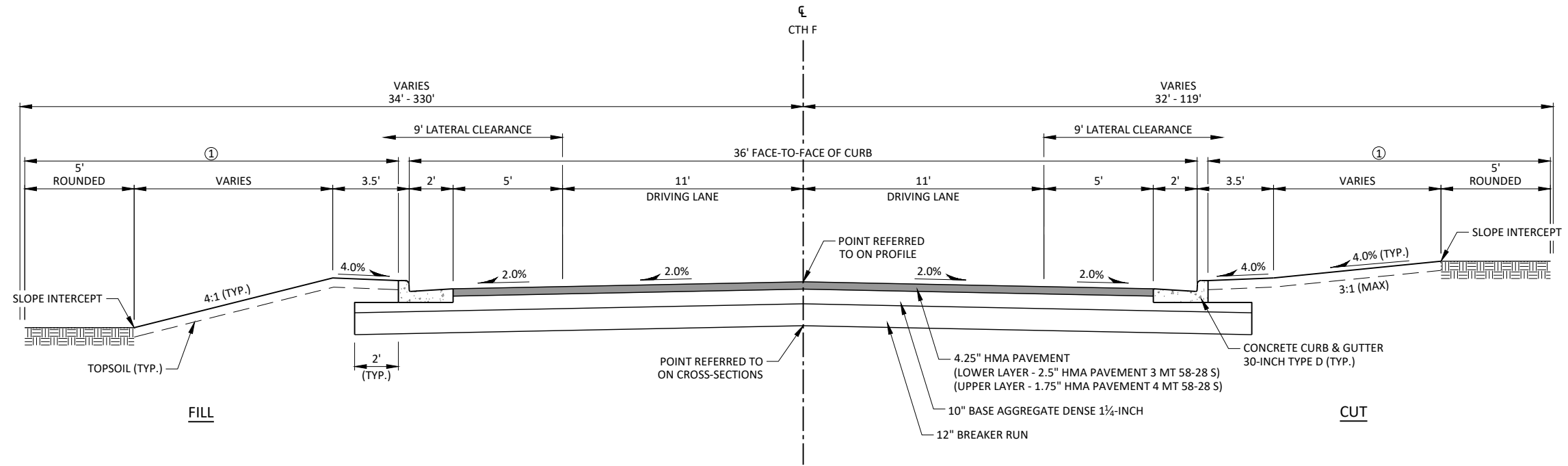
TYPICAL EXISTING SECTION

ROADWAY	(A) (FT.)	(B) (FT.)	(C)	(D)	(E)	(F)
'B'-LINE (MOUND STREET)	11	2	VARIES 1%-3%	VARIES 1%-5%	DEPTH UNKNOWN	DEPTH UNKNOWN
'E'-LINE (HORSE SHOE BEND RD.)	VARIES 11-12	VARIES 0-1	VARIES 4%-6%	VARIES 4%-6%	DEPTH UNKNOWN	DEPTH UNKNOWN
'F'-LINE (WEST MOSCOW RD.)	11	VARIES 0-1	VARIES 2%-4%	VARIES 4%-6%	DEPTH UNKNOWN	DEPTH UNKNOWN
'H'-LINE (MCKENNA RD.)	10	VARIES 0-2	VARIES 2%-3%	VARIES 5%-7%	DEPTH UNKNOWN	DEPTH UNKNOWN
'I'-LINE (EAST PECATONICA RD.)	10	VARIES 0-2	VARIES 2%-3%	VARIES 2%-3%	1"	11"



TYPICAL EXISTING SECTION

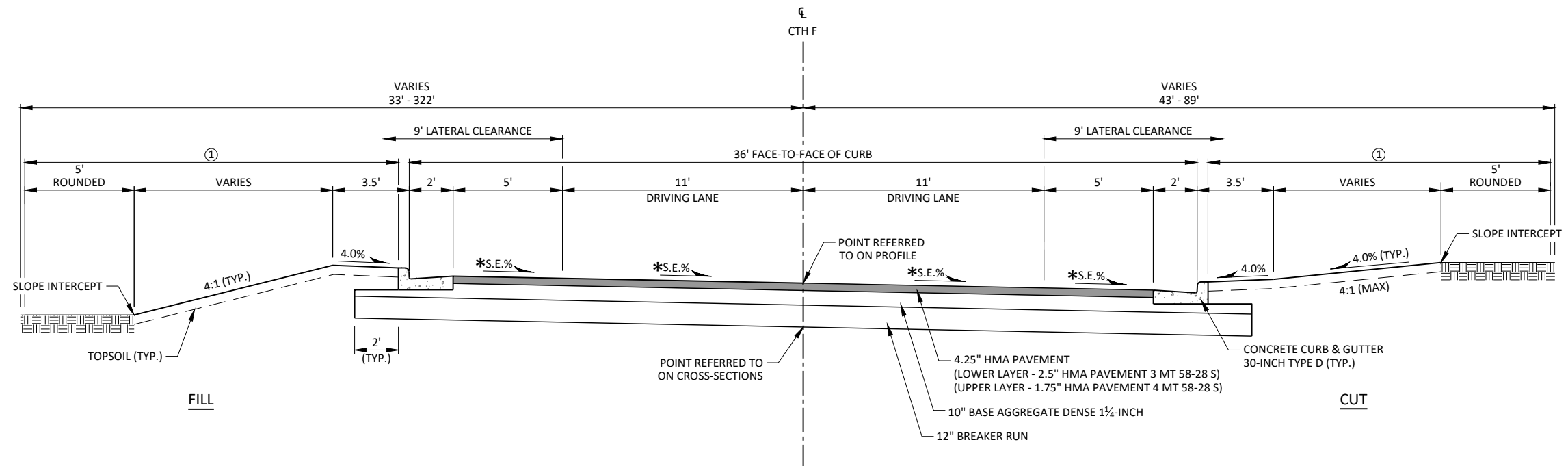
'J'-LINE
(STH 39)



TYPICAL FINISHED SECTION

CTH F
 STA. 10+24 - STA. 12+44
 STA. 16+22 - STA. 20+69
 STA. 24+72 - STA. 27+82

① LIMITS OF FERTILIZER TYPE B, SEEDING MIXTURE NO. 40, TOPSOIL & MULCH/EROSION MAT (SEE EROSION CONTROL PLAN FOR FURTHER INFORMATION FOR MULCH/EROSION MAT LIMITS) (AS DIRECTED BY ENGINEER)

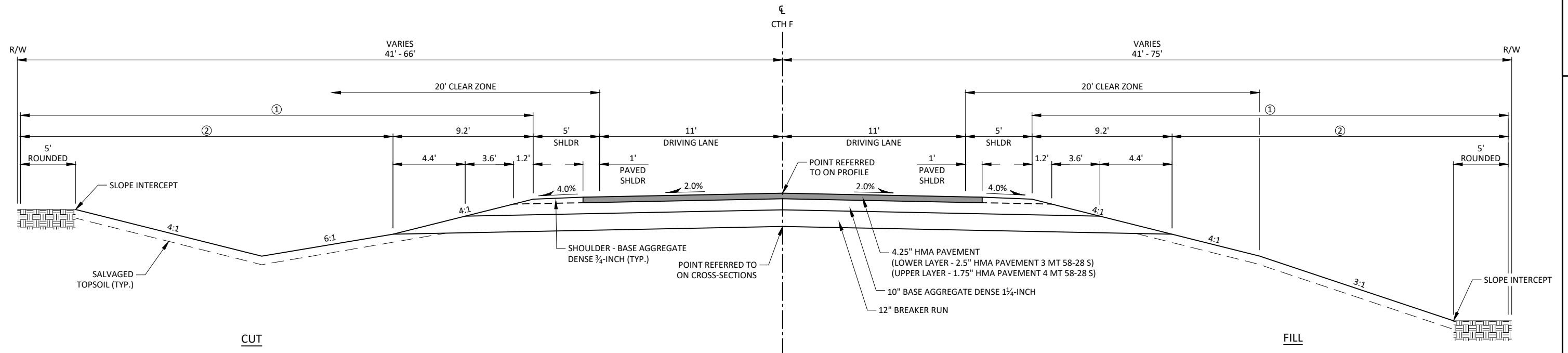


TYPICAL FINISHED SUPERELEVATED SECTION

CTH F
 STA. 12+44 - STA. 16+22
 STA. 20+69 - STA. 24+72
 STA. 27+82 - STA. 30+75, LT.
 STA. 27+82 - STA. 29+50, RT.

* SEE SUPERELEVATION TABLE

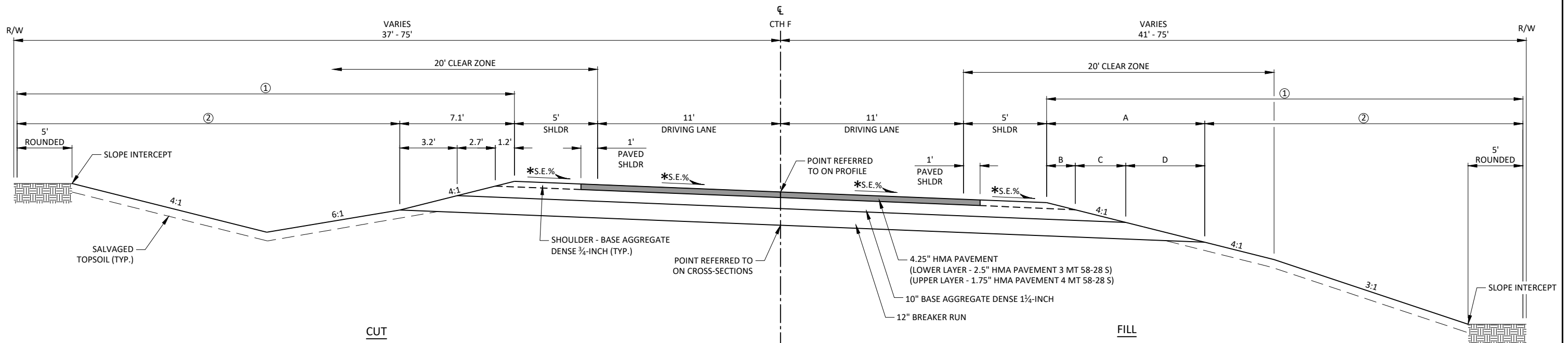
① LIMITS OF FERTILIZER TYPE B, SEEDING MIXTURE NO. 40, TOPSOIL & MULCH/EROSION MAT (SEE EROSION CONTROL PLAN FOR FURTHER INFORMATION FOR MULCH/EROSION MAT LIMITS) (AS DIRECTED BY ENGINEER)



TYPICAL FINISHED SECTION

CTH F
 STA. 43+92 - STA. 50+00 STA. 186+50 - STA. 191+00
 STA. 174+28 - STA. 179+70, RT. STA. 216+44 - STA. 218+93
 STA. 174+50 - STA. 179+70, LT. STA. 222+72 - STA. 224+09

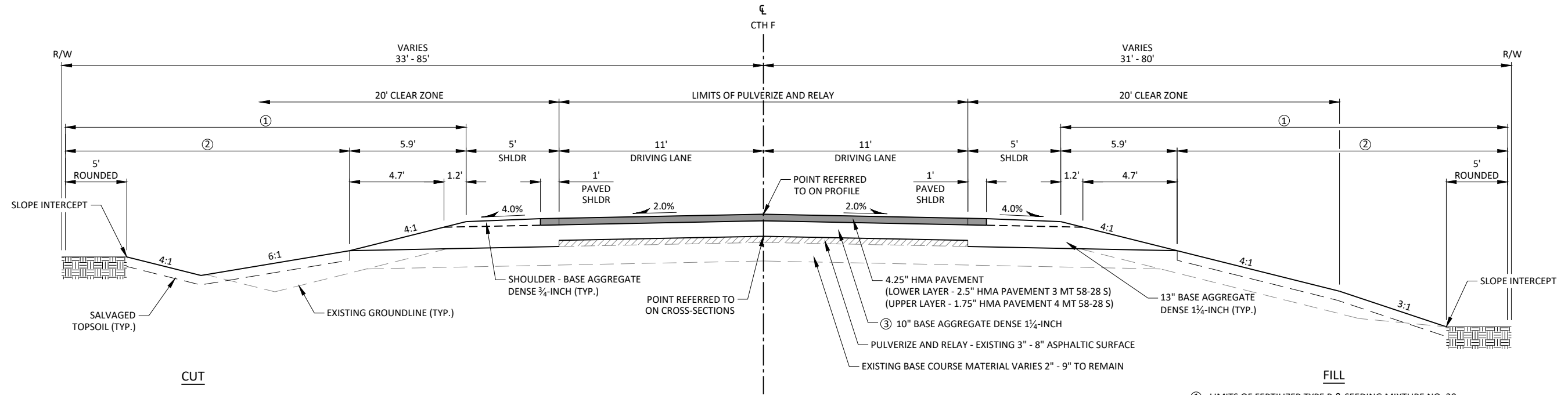
- ① LIMITS OF FERTILIZER TYPE B & SEEDING MIXTURE NO. 20 (AS DIRECTED BY ENGINEER)
- ② LIMITS OF SALVAGED TOPSOIL & MULCH/EROSION MAT (SEE EROSION CONTROL PLAN FOR FURTHER INFORMATION FOR MULCH/EROSION MAT LIMITS) (AS DIRECTED BY ENGINEER)



TYPICAL FINISHED SUPERELEVATED SECTION

CTH F
 STA. 29+50 - STA. 43+92, RT. STA. 218+93 - STA. 222+72
 STA. 30+75 - STA. 43+92, LT. STA. 224+09 - STA. 226+80
 STA. 168+73 - STA. 170+66 STA. 257+50 - STA. 259+25, RT
 STA. 170+66 - STA. 174+28, RT. STA. 257+50 - STA. 259+50, LT
 STA. 211+10 - STA. 216+44 STA. 261+50 - STA. 264+61.40, LT

- * SEE SUPERELEVATION TABLE
- ① LIMITS OF FERTILIZER TYPE B & SEEDING MIXTURE NO. 20 (AS DIRECTED BY ENGINEER)
- ② LIMITS OF SALVAGED TOPSOIL & MULCH/EROSION MAT (SEE EROSION CONTROL PLAN FOR FURTHER INFORMATION FOR MULCH/EROSION MAT LIMITS) (AS DIRECTED BY ENGINEER)



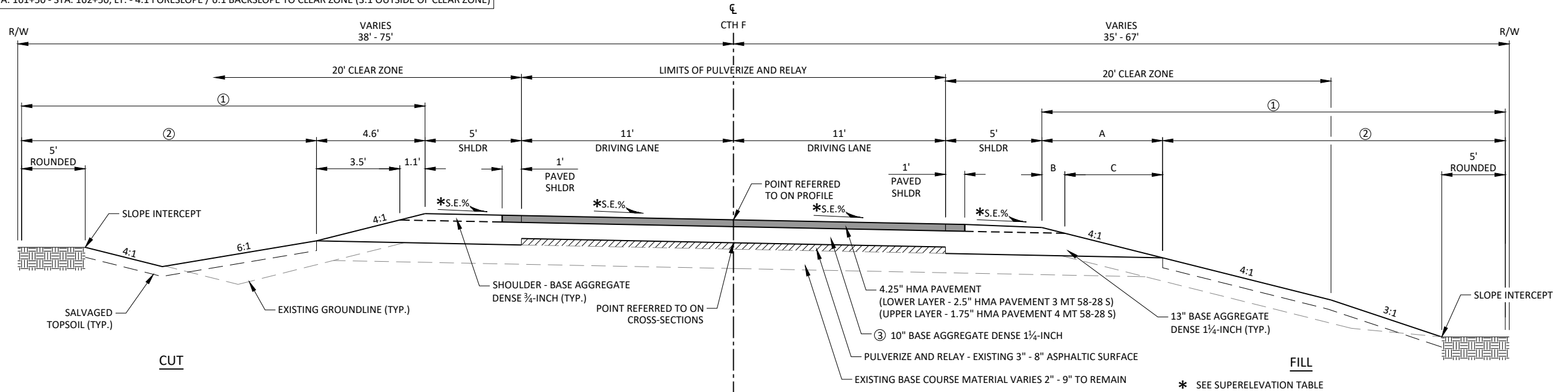
TYPICAL FINISHED SECTION

CTH F

STA. 50+00 - STA. 95+98, LT.	STA. 137+96 - STA. 143+59
STA. 50+00 - STA. 53+30, RT.	STA. 151+31 - STA. 151+95
STA. 60+50 - STA. 95+98, RT.	STA. 159+70 - STA. 161+25
STA. 102+58 - STA. 104+64	STA. 179+70 - STA. 186+50
STA. 108+17 - STA. 108+35	STA. 191+00 - STA. 209+84
STA. 115+04 - STA. 115+80	STA. 230+37 - STA. 235+30
STA. 124+57 - STA. 128+31	STA. 239+85 - STA. 247+59
STA. 132+47 - STA. 133+20	

- ① LIMITS OF FERTILIZER TYPE B & SEEDING MIXTURE NO. 20 (AS DIRECTED BY ENGINEER)
- ② LIMITS OF SALVAGED TOPSOIL & MULCH/EROSION MAT (SEE EROSION CONTROL PLAN FOR FURTHER INFORMATION FOR MULCH/EROSION MAT LIMITS) (AS DIRECTED BY ENGINEER)
- ③ PLACE 4" BASE AGGREGATE DENSE 1 1/4-INCH ON TOP OF EXISTING ASPHALTIC SURFACE PRIOR TO PULVERIZE AND RELAY OPERATIONS. POST PULVERIZE AND RELAY OPERATIONS PLACE REMAINING 6" BASE AGGREGATE DENSE 1 1/4-INCH .

STA. 116+50 - STA. 117+75, LT. - SEE CROSS SECTIONS FOR SIDESLOPES
 STA. 124+50 - STA. 126+50, LT. - 6:1 FORESLOPE / 4:1 BACKSLOPE TO CLEAR ZONE (3:1 OUTSIDE OF CLEAR ZONE)
 STA. 139+50 - STA. 140+50, LT. - 6:1 FORESLOPE / 4:1 BACKSLOPE TO CLEAR ZONE (3:1 OUTSIDE OF CLEAR ZONE)
 STA. 141+50 - STA. 142+50, LT. - 6:1 FORESLOPE / 4:1 BACKSLOPE TO CLEAR ZONE (3:1 OUTSIDE OF CLEAR ZONE)
 STA. 161+50 - STA. 162+50, LT. - 4:1 FORESLOPE / 6:1 BACKSLOPE TO CLEAR ZONE (3:1 OUTSIDE OF CLEAR ZONE)

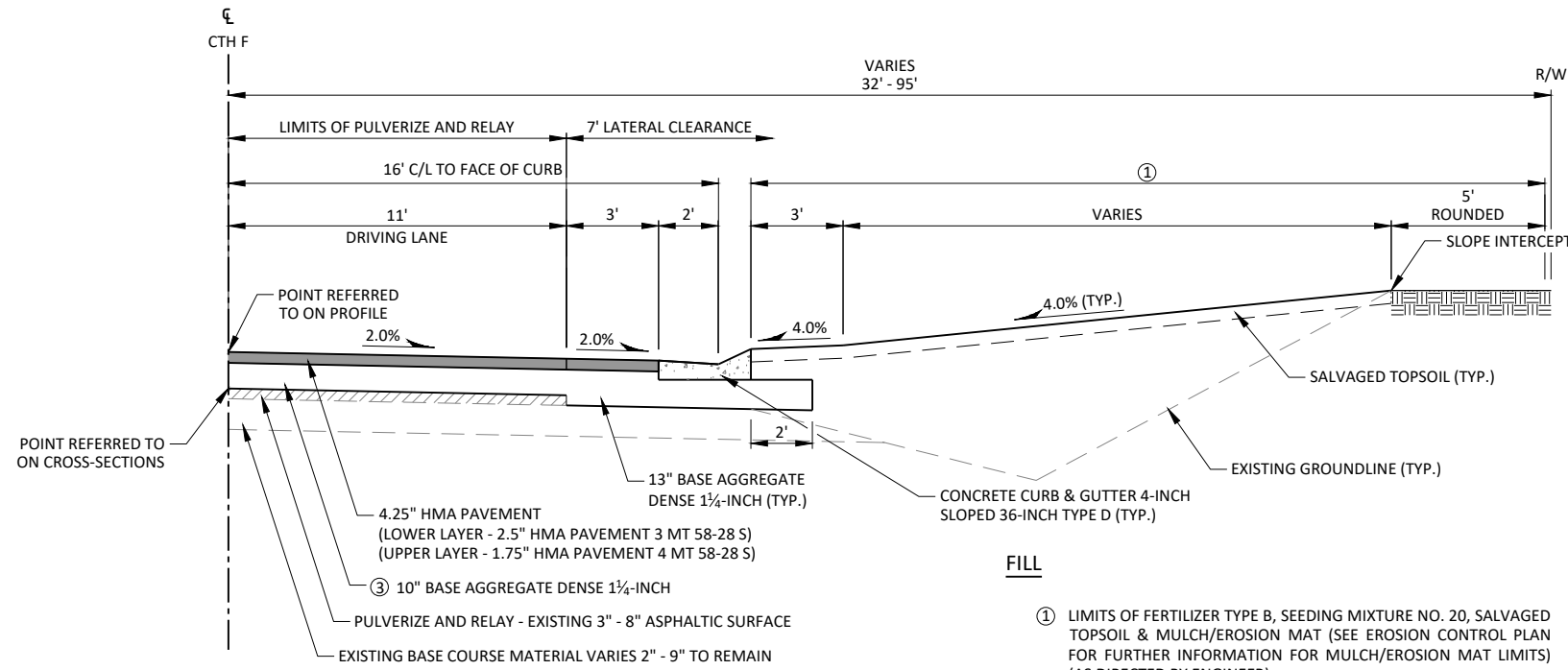


TYPICAL FINISHED SUPERELEVATED SECTION

CTH F

STA. 95+98 - STA. 102+58	STA. 143+59 - STA. 151+31
STA. 104+64 - STA. 108+17	STA. 151+95 - STA. 159+70
STA. 108+35 - STA. 115+04, RT.	STA. 161+25 - STA. 168+73
STA. 108+35 - STA. 109+00, LT.	STA. 209+84 - STA. 211+10
STA. 114+00 - STA. 115+04, LT.	STA. 226+80 - STA. 230+37
STA. 115+80 - STA. 124+57	STA. 235+30 - STA. 239+85
STA. 128+31 - STA. 132+47	STA. 247+59 - STA. 257+50
STA. 133+20 - STA. 137+96	

- * SEE SUPERELEVATION TABLE
- ① LIMITS OF FERTILIZER TYPE B & SEEDING MIXTURE NO. 20 (AS DIRECTED BY ENGINEER)
- ② LIMITS OF SALVAGED TOPSOIL & MULCH/EROSION MAT (SEE EROSION CONTROL PLAN FOR FURTHER INFORMATION FOR MULCH/EROSION MAT LIMITS) (AS DIRECTED BY ENGINEER)
- ③ PLACE 4" BASE AGGREGATE DENSE 1 1/4-INCH ON TOP OF EXISTING ASPHALTIC SURFACE PRIOR TO PULVERIZE AND RELAY OPERATIONS. POST PULVERIZE AND RELAY OPERATIONS PLACE REMAINING 6" BASE AGGREGATE DENSE 1 1/4-INCH .

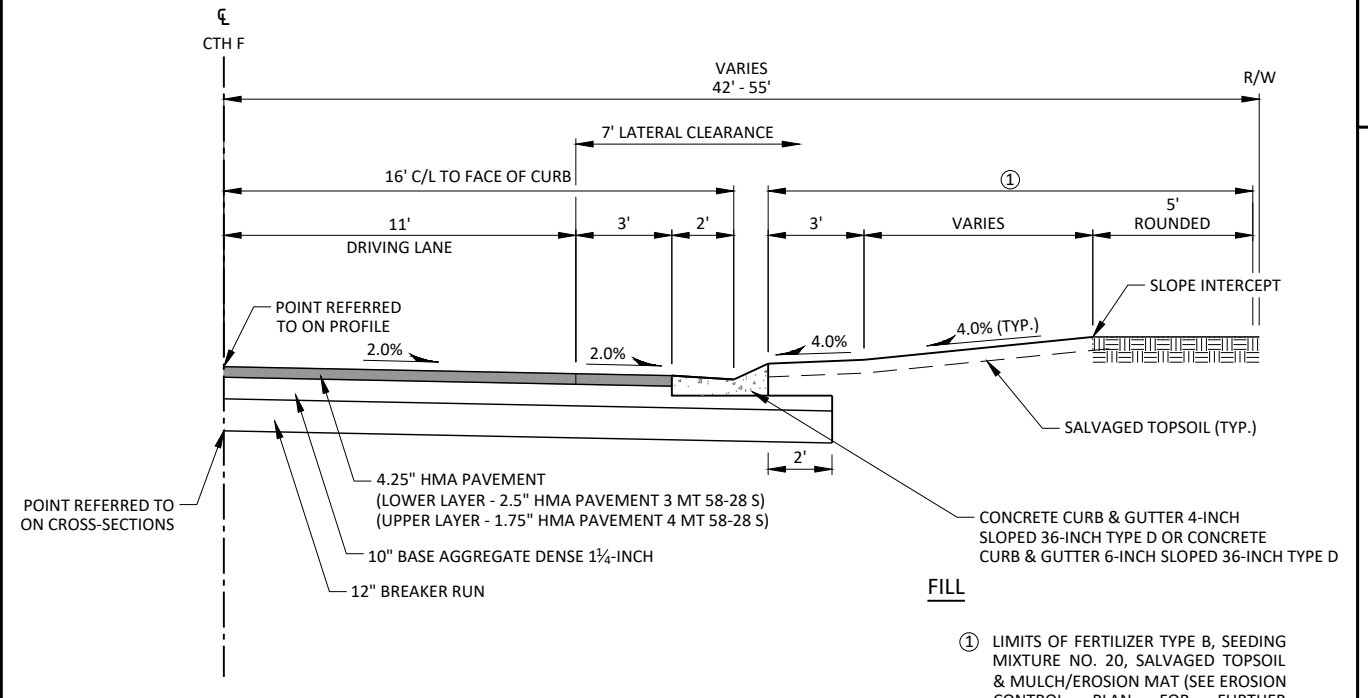


TYPICAL FINISHED HALF SECTION

CTH F
STA. 53+30 - STA. 60+50, RT.

FILL

- ① LIMITS OF FERTILIZER TYPE B, SEEDING MIXTURE NO. 20, SALVAGED TOPSOIL & MULCH/EROSION MAT (SEE EROSION CONTROL PLAN FOR FURTHER INFORMATION FOR MULCH/EROSION MAT LIMITS) (AS DIRECTED BY ENGINEER)
- ③ PLACE 4" BASE AGGREGATE DENSE 1 1/4-INCH ON TOP OF EXISTING ASPHALTIC SURFACE PRIOR TO PULVERIZE AND RELAY ACTIVITIES. POST PULVERIZE AND RELAY OPERATIONS PLACE REMAINING 6" BASE AGGREGATE DENSE 1 1/4-INCH .

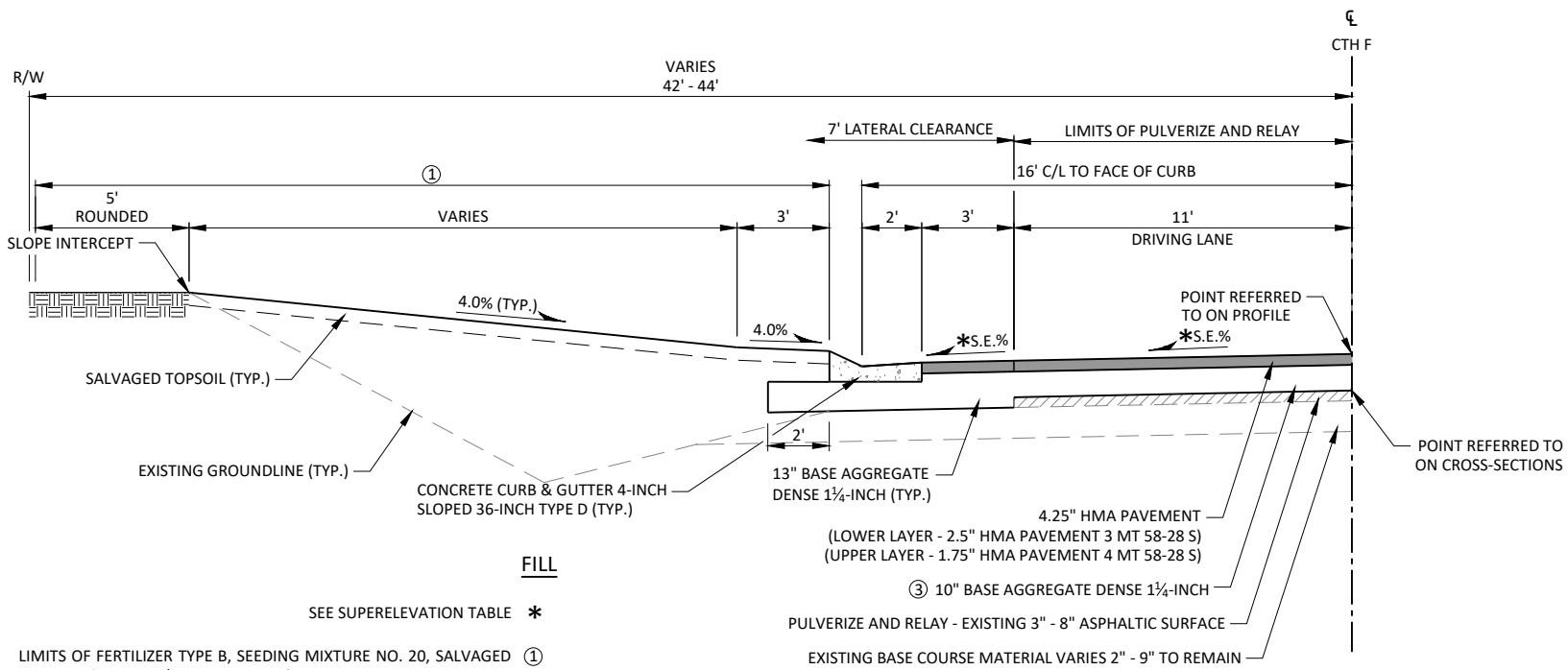


TYPICAL FINISHED HALF SECTION

CTH F
STA. 174+28 - STA. 174+50, LT.
STA. 264+61.40 - STA. 265+24.10

FILL

- ① LIMITS OF FERTILIZER TYPE B, SEEDING MIXTURE NO. 20, SALVAGED TOPSOIL & MULCH/EROSION MAT (SEE EROSION CONTROL PLAN FOR FURTHER INFORMATION FOR MULCH/EROSION MAT LIMITS) (AS DIRECTED BY ENGINEER)

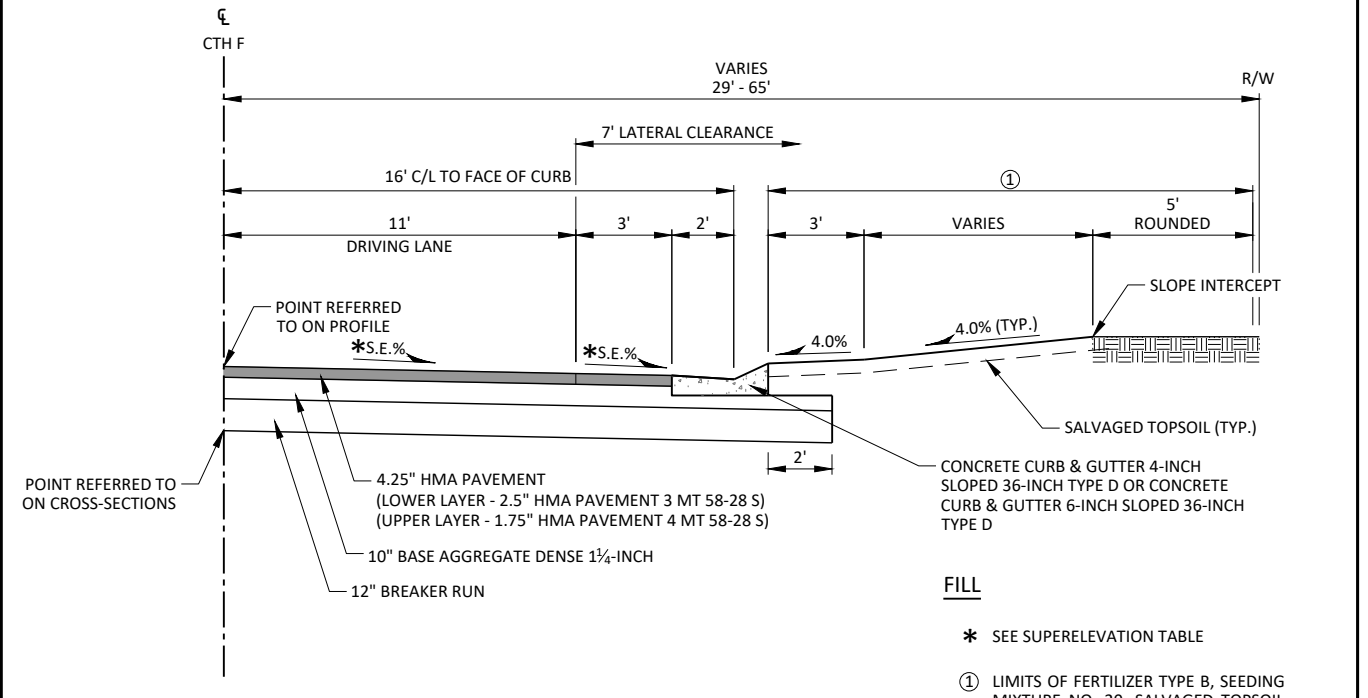


TYPICAL FINISHED SUPERELEVATED HALF SECTION

CTH F
STA. 109+00 - STA. 114+00, LT.

FILL

- * SEE SUPERELEVATION TABLE
- ① LIMITS OF FERTILIZER TYPE B, SEEDING MIXTURE NO. 20, SALVAGED TOPSOIL & MULCH/EROSION MAT (SEE EROSION CONTROL PLAN FOR FURTHER INFORMATION FOR MULCH/EROSION MAT LIMITS) (AS DIRECTED BY ENGINEER)
- ③ PLACE 4" BASE AGGREGATE DENSE 1 1/4-INCH ON TOP OF EXISTING ASPHALTIC SURFACE PRIOR TO PULVERIZE AND RELAY ACTIVITIES. POST PULVERIZE AND RELAY OPERATIONS PLACE REMAINING 6" BASE AGGREGATE DENSE 1 1/4-INCH .

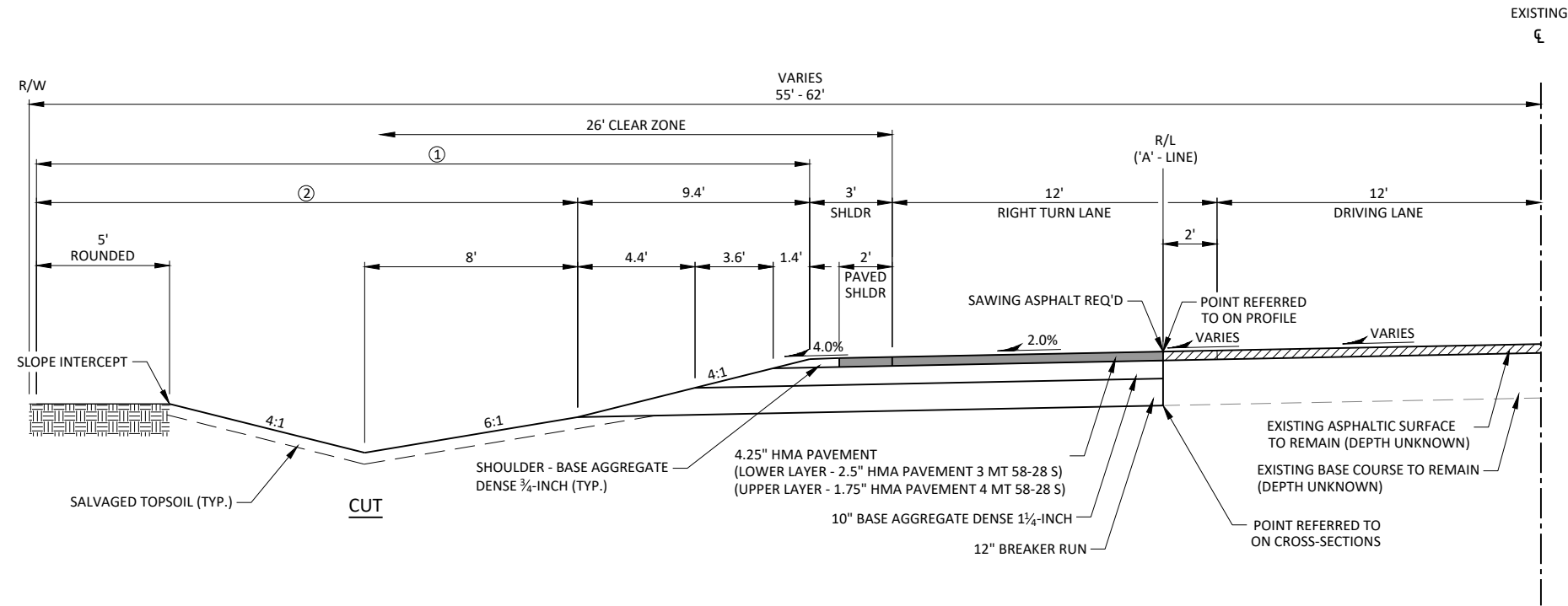


TYPICAL FINISHED SUPERELEVATED HALF SECTION

CTH F
STA. 170+67 - STA. 174+28, LT.
STA. 259+25 - STA. 265+24.10, RT.
STA. 259+50 - STA. 261+50, LT.

FILL

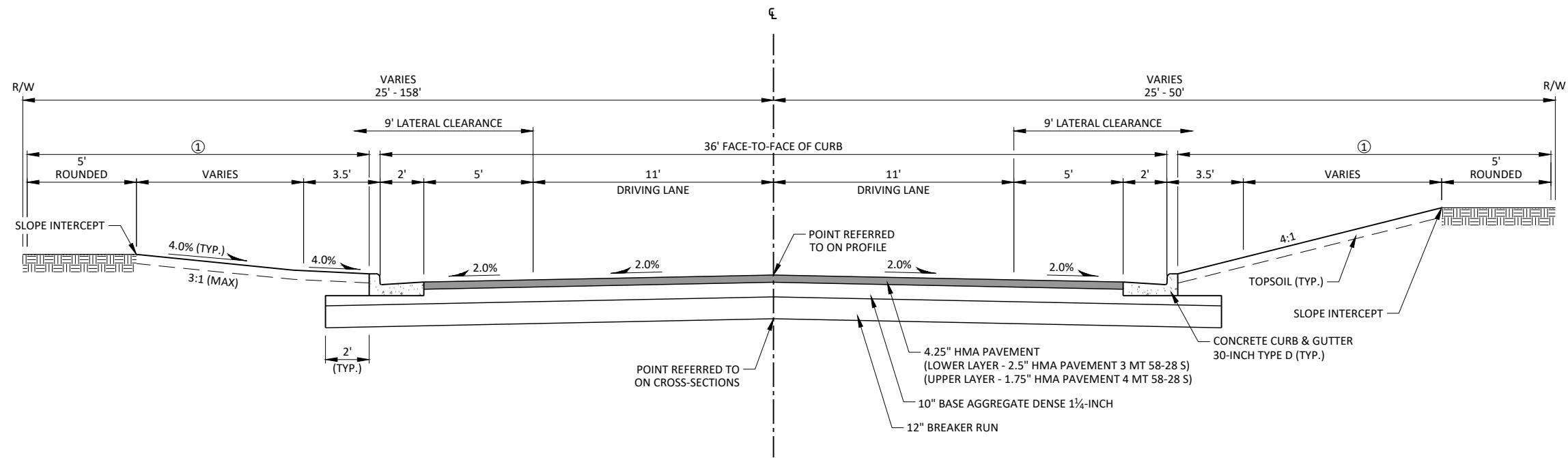
- * SEE SUPERELEVATION TABLE
- ① LIMITS OF FERTILIZER TYPE B, SEEDING MIXTURE NO. 20, SALVAGED TOPSOIL & MULCH/EROSION MAT (SEE EROSION CONTROL PLAN FOR FURTHER INFORMATION FOR MULCH/EROSION MAT LIMITS) (AS DIRECTED BY ENGINEER)



TYPICAL FINISHED HALF SECTION 'A'-LINE (STH 78)

'A'-LINE
(STH 78)
STA. 399'A'+55 - STA. 406'A'+98.30

- ① LIMITS OF FERTILIZER TYPE B, SEEDING MIXTURE NO. 40 (AS DIRECTED BY ENGINEER)
- ② LIMITS OF TOPSOIL AND MULCH/EROSION MAT (SEE EROSION CONTROL PLAN FOR FURTHER INFORMATION FOR MULCH/EROSION MAT LIMITS) (AS DIRECTED BY ENGINEER)



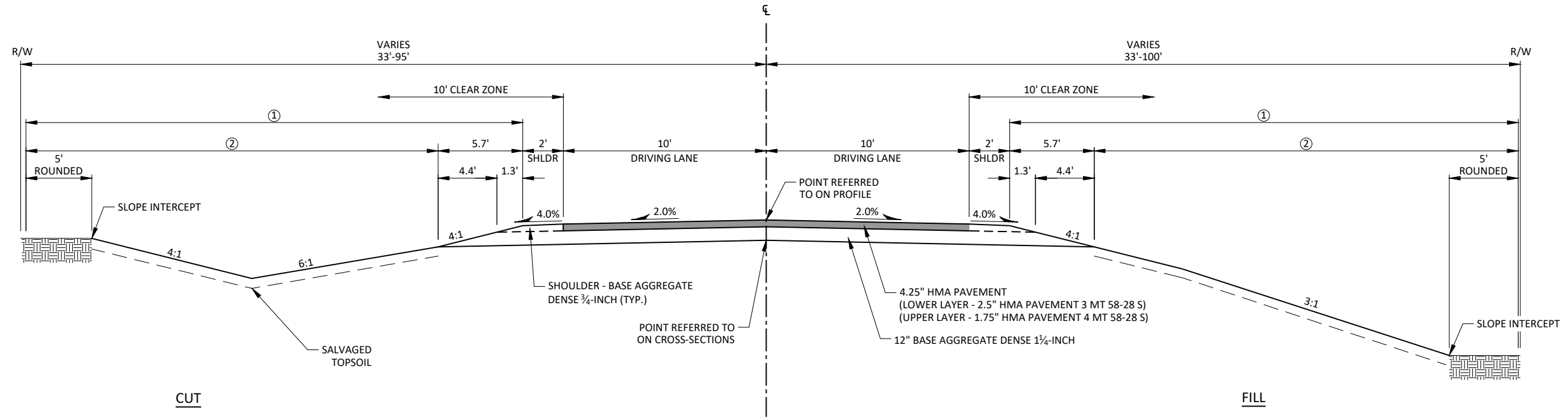
TYPICAL FINISHED SECTION

LINE
'B'-LINE
'O'-LINE

SIDEROAD
MOUND STREET
N. MAIN STREET

STATION - STATION
399'B'+62.87 - 402'B'+03.52
10'O'+00 - 10'O'+61.86

- ① LIMITS OF FERTILIZER TYPE B, SEEDING MIXTURE NO. 40, TOPSOIL & MULCH/EROSION MAT (SEE EROSION CONTROL PLAN FOR FURTHER INFORMATION FOR MULCH/EROSION MAT LIMITS) (AS DIRECTED BY ENGINEER)



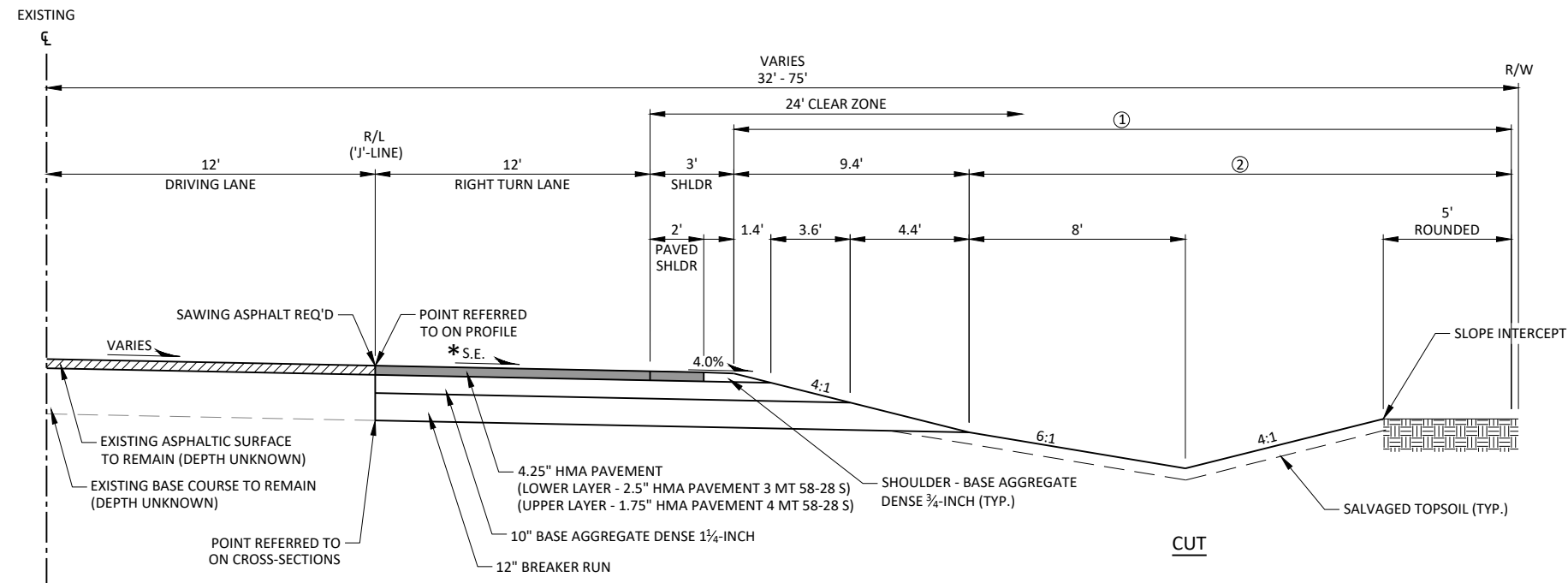
TYPICAL FINISHED SECTION

LINE
'E'-LINE
'F'-LINE
'H'-LINE
'I'-LINE

SIDE ROAD
HORSESHOE BEND ROAD
WEST MOSCOW ROAD
MCKENNA ROAD
EAST PECATONICA ROAD

STATION - STATION
401'E+35 - 403'E+24.08
400'F+16 - 402'F+20
400'H+20 - 404'H+28.79
401'I+70 - 409'I+24.87

- ① LIMITS OF FERTILIZER TYPE B & SEEDING MIXTURE NO. 40 (AS DIRECTED BY ENGINEER)
- ② LIMITS OF SALVAGED TOPSOIL & MULCH/EROSION MAT (SEE EROSION CONTROL PLAN FOR FURTHER INFORMATION FOR MULCH/EROSION MAT LIMITS) (AS DIRECTED BY ENGINEER)



TYPICAL FINISHED HALF SECTION

'J'-LINE
(STH 39)
STA. 400'J+50 - STA. 407'J+47.45

- ① LIMITS OF FERTILIZER TYPE B, SEEDING MIXTURE NO. 40 (AS DIRECTED BY ENGINEER)
- ② LIMITS OF SALVAGED TOPSOIL & MULCH/EROSION MAT (SEE EROSION CONTROL PLAN FOR FURTHER INFORMATION FOR MULCH/EROSION MAT LIMITS) (AS DIRECTED BY ENGINEER)
- * SEE SUPERELEVATION TABLE

SUPERELEVATION TABLE-CURVE 1

Table with columns: STATION, LEFT(%), RIGHT(%). Rows include stations 12+44 to 16+22 and a FULL SUPERELEVATION section.

SUPERELEVATION TABLE-CURVE 5

Table with columns: STATION, LEFT(%), RIGHT(%), A (FT), B (FT), C (FT). Rows include stations 95+98 to 102+58 and a FULL SUPERELEVATION section.

SUPERELEVATION TABLE-CURVE 8

Table with columns: STATION, LEFT(%), RIGHT(%), A (FT), B (FT), C (FT). Rows include stations 115+80 to 124+57 and a FULL SUPERELEVATION section.

SUPERELEVATION TABLE-CURVE 12

MAINTAIN NORMAL CROWN THROUGH CURVE 12

SUPERELEVATION TABLE-CURVE 13

Table with columns: STATION, LEFT(%), RIGHT(%), A (FT), B (FT), C (FT). Rows include stations 143+59 to 151+31 and a FULL SUPERELEVATION section.

SUPERELEVATION TABLE-CURVE 2

Table with columns: STATION, LEFT(%), RIGHT(%). Rows include stations 20+69 to 24+72 and a FULL SUPERELEVATION section.

SUPERELEVATION TABLE-CURVE 6

Table with columns: STATION, LEFT(%), RIGHT(%), A (FT), B (FT), C (FT). Rows include stations 104+64 to 108+17 and a FULL SUPERELEVATION section.

SUPERELEVATION TABLE-CURVE 10

Table with columns: STATION, LEFT(%), RIGHT(%), A (FT), B (FT), C (FT). Rows include stations 128+31 to 132+47 and a FULL SUPERELEVATION section.

SUPERELEVATION TABLE-CURVE 14

Table with columns: STATION, LEFT(%), RIGHT(%), A (FT), B (FT), C (FT). Rows include stations 151+95 to 159+70 and a FULL SUPERELEVATION section.

SUPERELEVATION TABLE-CURVE 3

Table with columns: STATION, LEFT(%), RIGHT(%), A (FT), B (FT), C (FT), D (FT). Rows include stations 27+82 to 43+92 and a FULL SUPERELEVATION section.

STA. 29+50, RT. END CONCRETE CURB & GUTTER

SUPERELEVATION TABLE-CURVE 7

Table with columns: STATION, LEFT(%), RIGHT(%), A (FT), B (FT), C (FT). Rows include stations 108+35 to 115+04 and a FULL SUPERELEVATION section.

STA. 109+00, LT. START CONCRETE CURB & GUTTER

STA. 114+00, LT. END CONCRETE CURB & GUTTER

SUPERELEVATION TABLE-CURVE 11

Table with columns: STATION, LEFT(%), RIGHT(%), A (FT), B (FT), C (FT). Rows include stations 133+20 to 137+96 and a FULL SUPERELEVATION section.

SUPERELEVATION TABLE-CURVE 15

Table with columns: STATION, LEFT(%), RIGHT(%), A (FT), B (FT), C (FT). Rows include stations 161+25 to 168+22 and a FULL SUPERELEVATION section.

SUPERELEVATION TABLE-CURVE 4

MAINTAIN NORMAL CROWN THROUGH CURVE 4

SUPERELEVATION TABLE-CURVE 16

STATION	LEFT(%)	RIGHT(%)	A (FT)	B (FT)	C (FT)	D (FT)
168+73	6.0	6.0	7.6	1.8	5.8	-
FULL SUPERELEVATION						
172+36	6.0	6.0	11.5	1.8	4.4	5.3
172+50	5.4	5.4	11.2	1.8	4.3	5.1
173+00	3.3	3.3	10.1	1.6	3.9	4.6
173+50	1.3	2.0	9.2	1.2	3.6	4.4
174+00	0.8	2.0	9.2	1.2	3.6	4.4
174+28	2.0	2.0	9.2	1.2	3.6	4.4

SUPERELEVATION TABLE-CURVE 19

STATION	LEFT(%)	RIGHT(%)	A (FT)	B (FT)	C (FT)	D (FT)
224+09	2.0	2.0	9.2	1.2	3.6	4.4
224+50	2.0	0.2	9.2	1.2	3.6	4.4
225+00	2.1	2.1	9.2	1.2	3.6	4.4
225+50	4.4	4.4	10.6	1.7	4.0	4.9
225+86	6.0	6.0	11.5	1.8	4.4	5.3
FULL SUPERELEVATION						
228+60	6.0	6.0	7.6	1.8	5.8	-
229+00	4.2	4.2	6.9	1.7	5.2	-
229+50	2.0	2.0	5.9	1.2	4.7	-
230+00	2.0	0.3	5.9	1.2	4.7	-
230+37	2.0	2.0	5.9	1.2	4.7	-

SUPERELEVATION TABLE-CURVE 22

STATION	LEFT(%)	RIGHT(%)
FULL SUPERELEVATION		
257+77	6.0	6.0
258+00	5.0	5.0
258+50	2.7	2.7
259+00	0.5	0.5
259+10	0.0	0.0

SUPERELEVATION TABLE-CURVE 17

STATION	LEFT(%)	RIGHT(%)	A (FT)	B (FT)	C (FT)	D (FT)
209+84	2.0	2.0	5.9	1.2	4.7	-
210+00	1.4	2.0	5.9	1.2	4.7	-
210+50	0.6	2.0	5.9	1.2	4.7	-
211+00	2.5	2.5	6.2	1.4	4.8	-
211+50	4.5	4.5	10.7	1.7	4.1	4.9
211+58	4.8	4.8	10.8	1.7	4.1	5.0
FULL SUPERELEVATION						
214+70	4.8	4.8	10.8	1.7	4.1	5.0
215+00	3.6	3.6	10.2	1.6	3.9	4.7
215+50	1.7	2.0	9.2	1.2	3.6	4.4
216+00	0.3	2.0	9.2	1.2	3.6	4.4
216+44	2.0	2.0	9.2	1.2	3.6	4.4

SUPERELEVATION TABLE-CURVE 20

STATION	LEFT(%)	RIGHT(%)	A (FT)	B (FT)	C (FT)
235+30	2.0	2.0	5.9	1.2	4.7
235+50	2.0	1.2	5.9	1.2	4.7
236+00	2.0	0.7	5.9	1.2	4.7
236+50	2.7	2.7	6.3	1.4	4.9
FULL SUPERELEVATION					
238+65	2.7	2.7	6.3	1.4	4.9
239+00	2.0	1.3	5.9	1.2	4.7
239+50	2.0	0.6	5.9	1.2	4.7
239+85	2.0	2.0	5.9	1.2	4.7

SUPERELEVATION TABLE-CURVE 23

STATION	LEFT(%)	RIGHT(%)
259+10	0.0	0.0
259+50	1.6	1.6
260+00	3.5	3.5
260+50	5.5	5.5
260+63	6.0	6.0
FULL SUPERELEVATION		
262+57.40	6.0	6.0
263+00	4.3	4.3
263+50	2.4	2.4
264+00	2.0	0.4
264+50	2.0	1.6
264+61.38	2.0	2.0

— STA. 259+50, LT. START
 CONCRETE CURB & GUTTER
 — STA. 261+50, LT. END
 CONCRETE CURB & GUTTER
 — BEGIN CONCRETE CURB &
 GUTTER, LT.

SUPERELEVATION TABLE-CURVE 18

STATION	LEFT(%)	RIGHT(%)	A (FT)	B (FT)	C (FT)	D (FT)
218+93	2.0	2.0	9.2	1.2	3.6	4.4
219+00	2.0	1.7	9.2	1.2	3.6	4.4
219+50	2.0	0.2	9.2	1.2	3.6	4.4
220+00	2.2	2.2	9.3	1.2	3.7	4.4
220+50	4.2	4.2	10.5	1.7	4	4.8
220+69	4.9	4.9	10.9	1.7	4.2	5.0
FULL SUPERELEVATION						
220+96	4.9	4.9	10.9	1.7	4.2	5.0
221+00	4.8	4.8	10.8	1.7	4.1	5.0
221+50	2.8	2.8	9.7	1.4	3.8	4.5
222+00	2.0	0.8	9.2	1.2	3.6	4.4
222+50	2.0	1.1	9.2	1.2	3.6	4.4
222+72	2.0	2.0	9.2	1.2	3.6	4.4

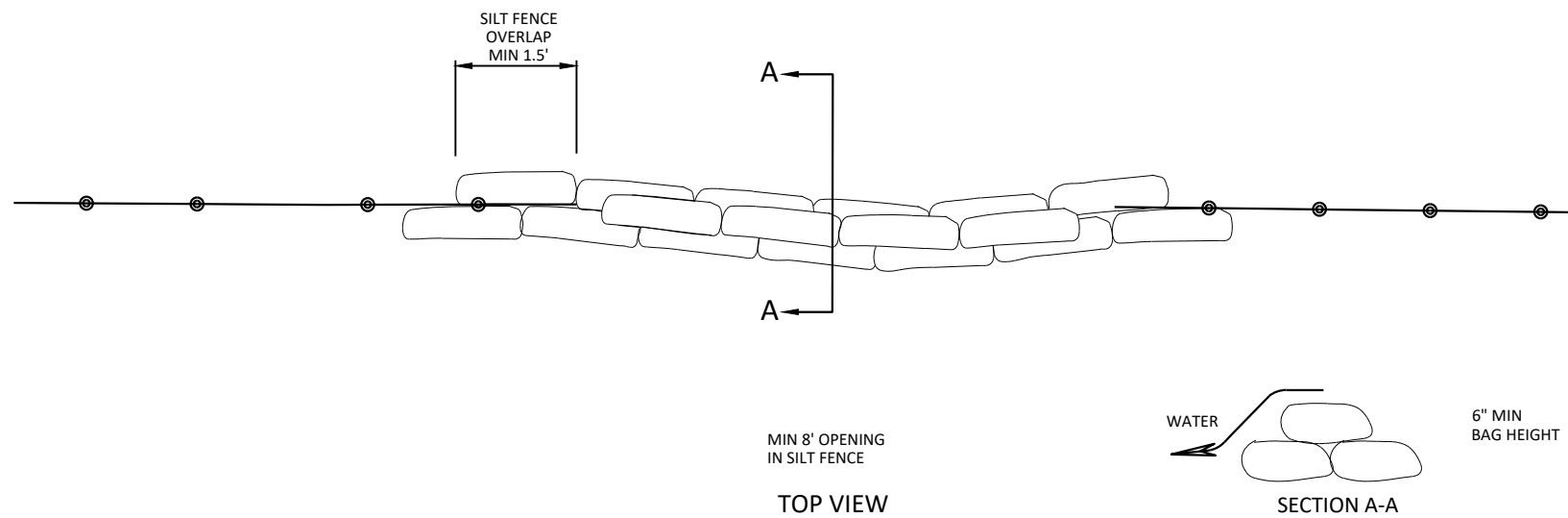
SUPERELEVATION TABLE-CURVE 21

STATION	LEFT(%)	RIGHT(%)
247+59	2.0	2.0
248+00	0.1	2.0
248+50	2.1	2.1
249+00	4.4	4.4
249+36	6.0	6.0
FULL SUPERELEVATION		

SUPERELEVATION TABLE-CURVE 32

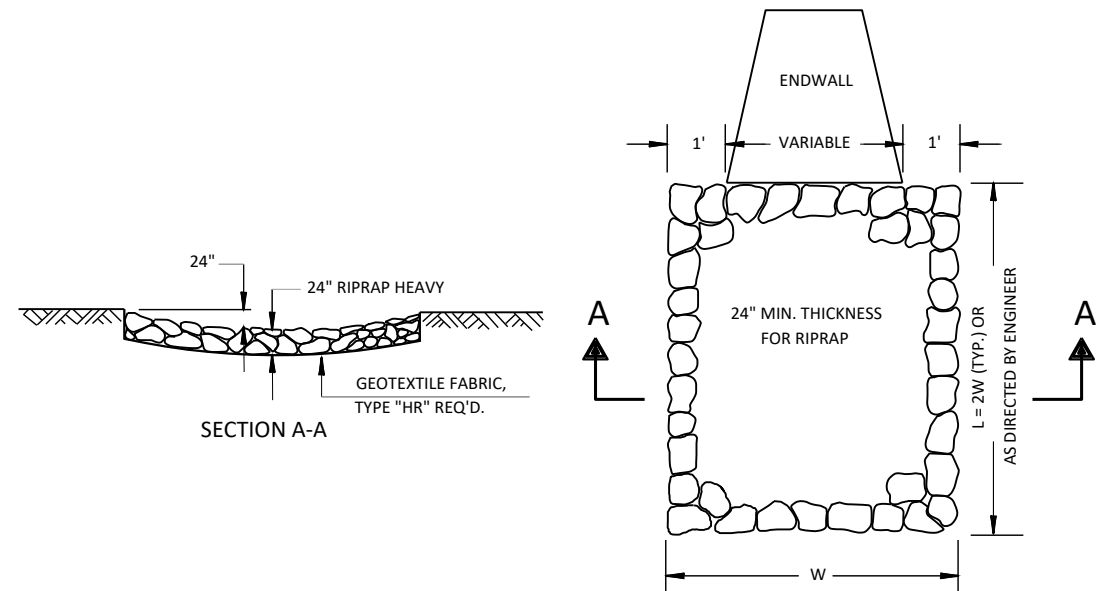
STATION	RIGHT(%)	A (FT)	B (FT)	C (FT)	D (FT)
400'+50	2.0	9.4	1.5	3.5	4.4
401'+00	2.0	9.4	1.5	3.5	4.4
401'+50	2.0	9.4	1.5	3.5	4.4
402'+00	2.0	9.4	1.5	3.5	4.4
402'+50	2.0	9.4	1.5	3.5	4.4
403'+00	2.0	9.4	1.5	3.5	4.4
403'+50	2.0	9.4	1.5	3.5	4.4
404'+00	2.0	9.4	1.5	3.5	4.4
404'+50	2.0	9.4	1.5	3.5	4.4
405'+00	2.0	-	-	-	-
405'+50	2.5	-	-	-	-
406'+00	4.6	-	-	-	-
406'+50	6.0	11.5	1.9	4.4	5.2
407'+00	6.0	11.5	1.9	4.4	5.2
407'+47.45	6.0	11.5	1.9	4.4	5.2

— CTH F/STH 78
 INTERSECTION

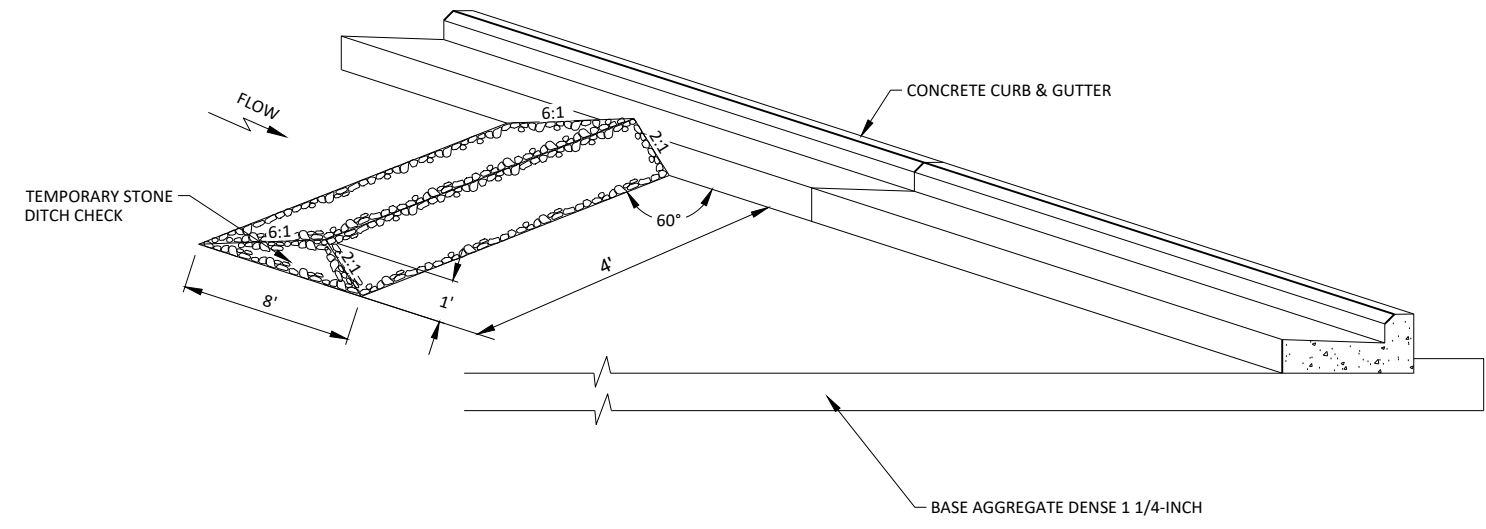


TOP VIEW
ROCK BAGS USED FOR SILT FENCE RELIEF
 (ROCK BAGS INCIDENTAL TO SILT FENCE BID ITEM)

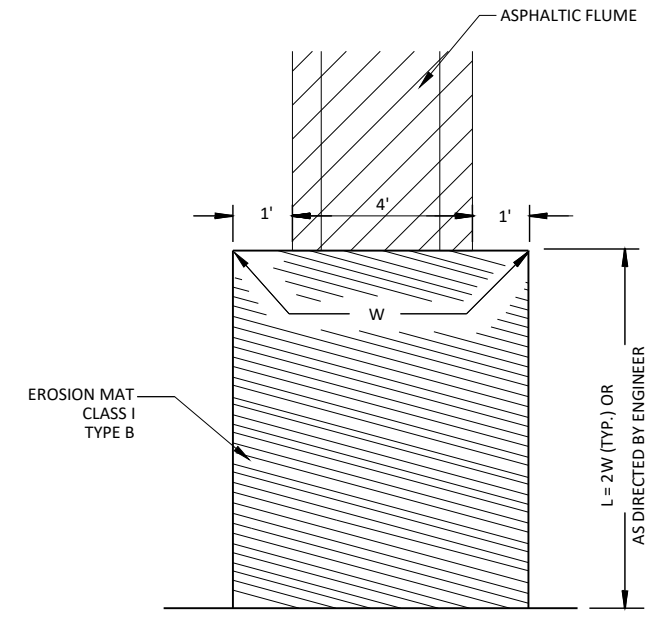
NOTE: LOCATION AND NUMBER OF ROCK BAGS TO BE DETERMINED BY THE ENGINEER IN THE FIELD.



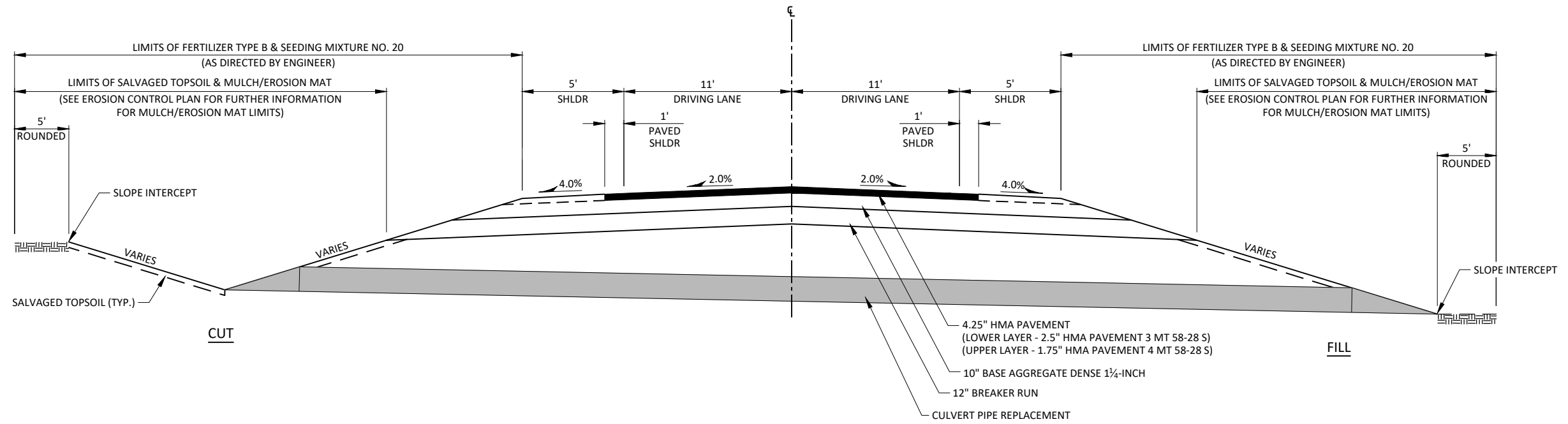
RIPRAP HEAVY TREATMENT AT CULVERTS



TEMPORARY STONE DITCH CHECKS
 (SEE MISCELLANEOUS QUANTITIES SHEET FOR LOCATION)



EROSION MAT CLASS I TYPE B TREATMENT AT ASPHALTIC FLUMES
 SEE EROSION CONTROL PLAN SHEET FOR LOCATION AND DIMENSIONS

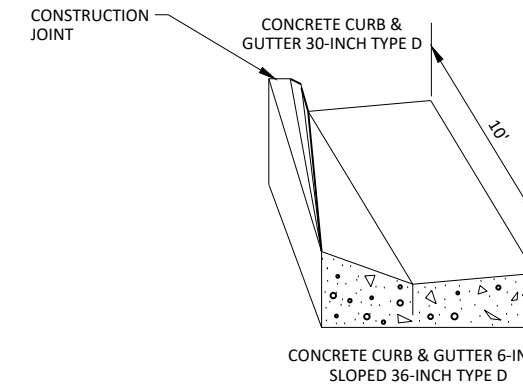
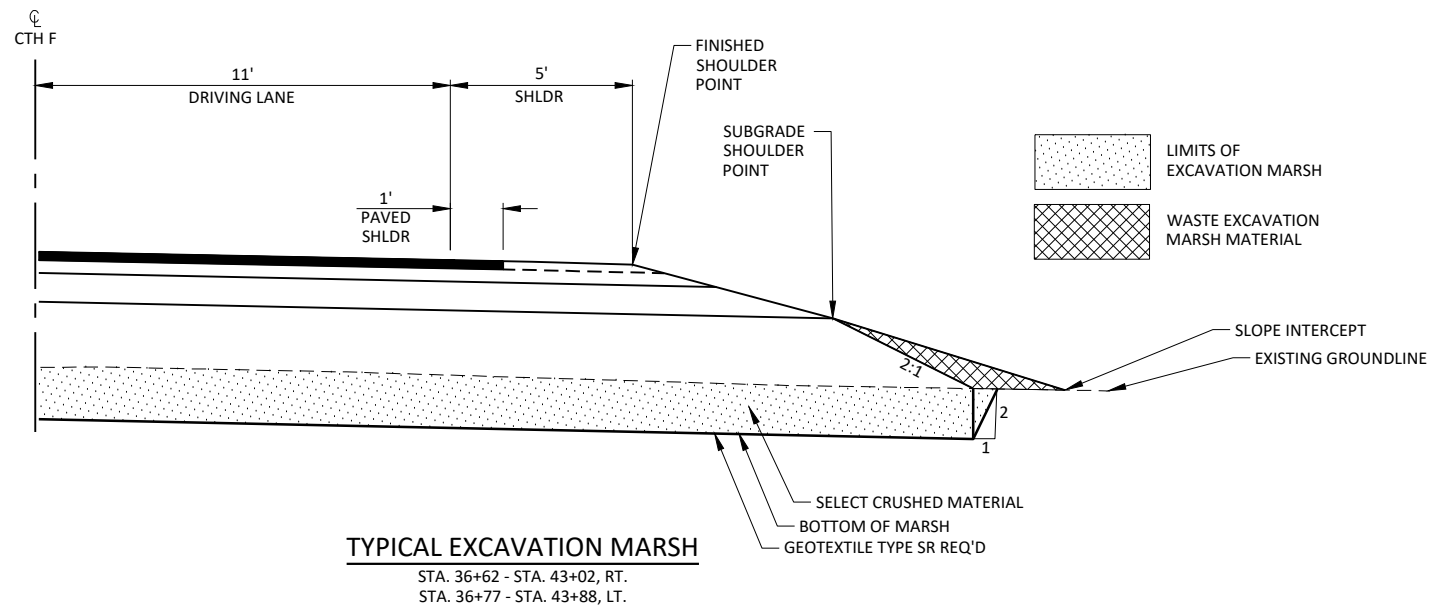


CULVERT REPLACEMENT DETAIL
(TYPICAL CULVERT REPLACEMENT IN PULVERIZE & RELAY SECTIONS)

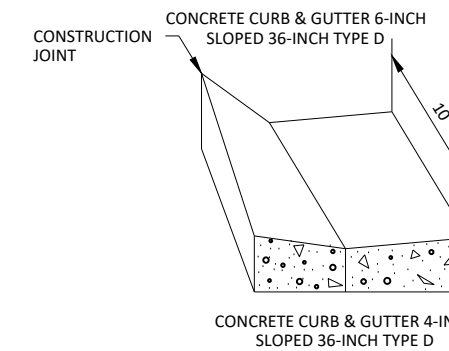
CULVERT LAYOUT TABLE

STATION	LOCATION	UPSTREAM				DOWNSTREAM			
		STATION	OFFSET	Y	X	STATION	OFFSET	Y	X
42+22	MAINLINE	42+22.92	37.97'	102,409.93	449,724.06	42+21.41	-44.02'	102,406.81	449,642.12
60+70	MAINLINE	60+71.90	34.95'	104,257.05	449,745.11	60+68.10	-34.95'	104,255.19	449,675.13
65+84	MAINLINE	65+84.16	25.00'	104,769.70	449,747.20	65+84.89	-35.00'	104,771.45	449,687.22
75+52	MAINLINE	75+53.07	23.99'	105,739.01	449,758.25	75+51.34	-35.99'	105,737.09	449,698.28
91+62	MAINLINE	91+62.03	-28.50'	107,347.63	449,693.52	91+61.54	25.50'	107,347.56	449,747.52
99+91	MAINLINE	99+88.30	-35.93'	108,168.89	449,65.08	99+93.15	43.92'	108,183.76	449,743.69
108+34	MAINLINE	108+34.40	-25.30'	108,998.02	449,514.79	108+34.40	26.70'	109,009.76	449,565.45
117+71	MAINLINE	117+71.24	-25.00'	109,629.30	448,916.19	117+71.24	27.00'	109,673.49	448,943.60
132+76	MAINLINE	132+76.77	23.97'	110,812.91	447,989.69	132+74.35	-25.97'	110,780.20	447,951.88
154+64	MAINLINE	154+60.70	31.86'	112,730.38	446,996.16	154+66.39	-27.87'	112,708.03	446,940.48
188+81	MAINLINE	188+82.98	-37.94'	115,550.07	445,441.53	188+77.85	53.92'	115,559.99	445,532.99
212+96	MAINLINE	212+76.14	-43.46'	117,909.96	445,030.08	213+26.35	61.22'	117,969.70	445,129.51
226+65	MAINLINE	226+63.35	33.97'	119,307.46	445,050.39	226+66.38	-31.96'	119,293.53	444,985.87
237+77	MAINLINE	237+72.35	35.74'	120,232.68	444,422.65	237+80.51	-31.77'	120,196.65	444,364.98
264+92	MAINLINE	264+92.53	33.00'	122,600.38	443,731.68	264+91.27	-46.99'	122,604.18	443,651.77
402'D'+30	'D'-LINE	402'D'+15.45	16.20'	101,639.88	450,037.59	402'D'+46.58	-17.67'	101,684.34	450,025.78
403'E'+09	'E'-LINE	403'E'+09.72	-56.04'	106,861.53	449,697.13	403'E'+08.19	46.01'	106,759.47	449,696.76
402'G'+25	'G'-LINE	402'G'+30.55	11.69'	109,517.46	449,311.47	402'G'+19.00	-11.61'	109,543.37	449,309.40
402'H'+00	'H'-LINE	402'H'+00.00	-22.00'	113,744.88	445,570.32	402'H'+00.00	26.00'	113,696.88	445,569.83
405'I'+00	'I'-LINE	405'I'+00.00	-50.00'	120,240.07	443,929.36	405'I'+00.00	66.00'	120,124.07	443,929.18

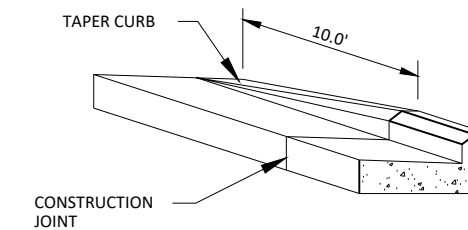
NOTE: P.E. / F.E. / C.E. CULVERT PIPES NOT INCLUDED IN TABLE.



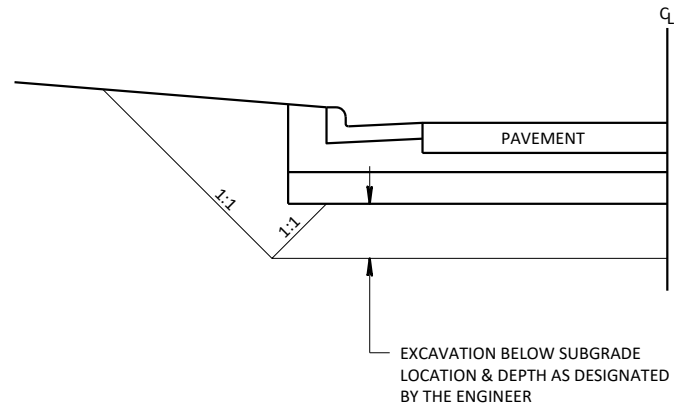
CURB & GUTTER TRANSITION DETAIL
 PAID FOR AS CONCRETE CURB & GUTTER 6-INCH SLOPED 36-INCH TYPE D
 (STA. 10+76 - STA. 10+86, LT)
 (STA. 10+84 - STA. 10+94, RT)



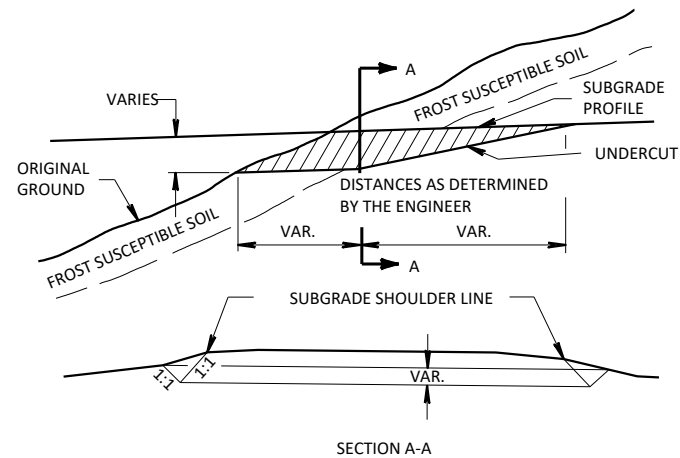
CURB & GUTTER TRANSITION DETAIL
 PAID FOR AS CONCRETE CURB & GUTTER 6-INCH SLOPED 36-INCH TYPE D
 (STA. 171+22 - STA. 171+32, LT)
 (STA. 264+50 - STA. 264+60, RT.)



DETAIL OF CURB & GUTTER TERMINI

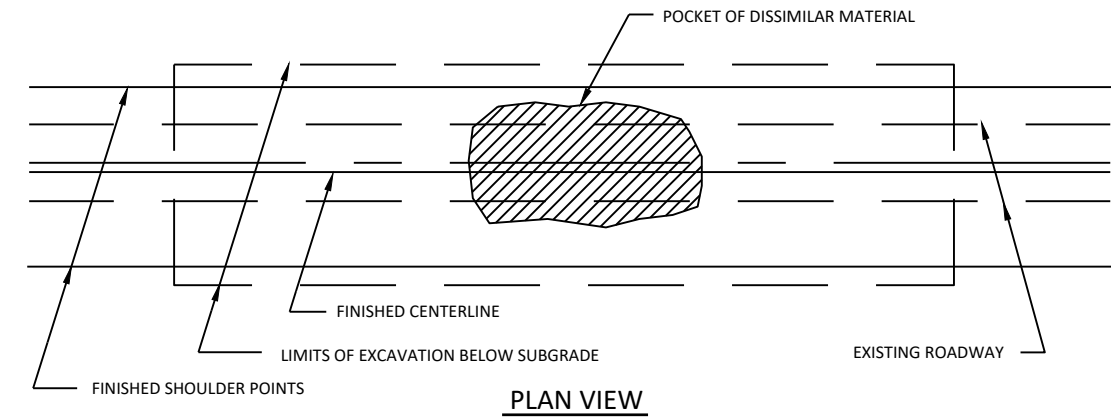


URBAN DETAIL FOR EXCAVATION BELOW SUBGRADE

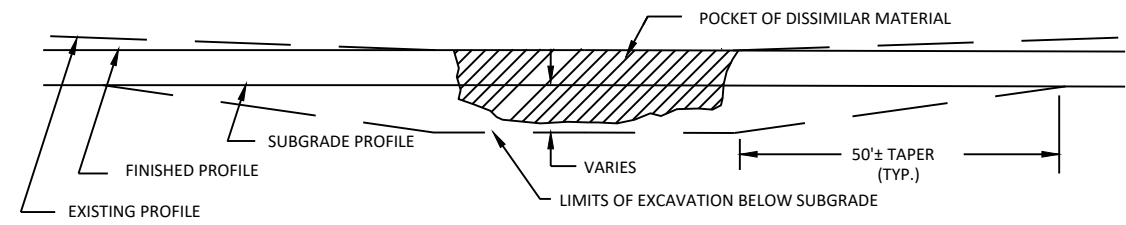


DETAIL FOR EXCAVATION BELOW SUBGRADE AT CUTS

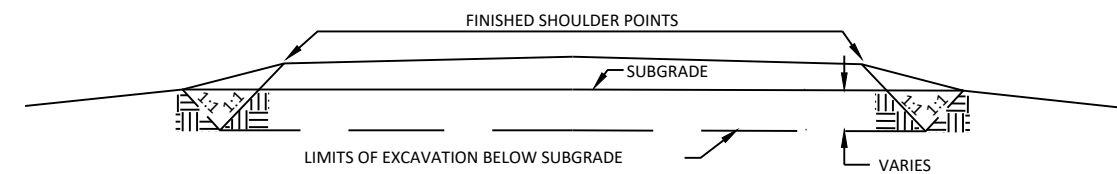
E.B.S. AREA TO BE BACKFILLED WITH BREAKER RUN.
 THE FILL SECTION WITHIN 100' OF THE MOUTH OF THE CUT MUST BE KEPT 2' BELOW SUBGRADE UNTIL E.B.S. IS COMPLETED.
 NOTE: EXACT LOCATIONS AND EXTENT OF E.B.S. SECTIONS SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD.



PLAN VIEW



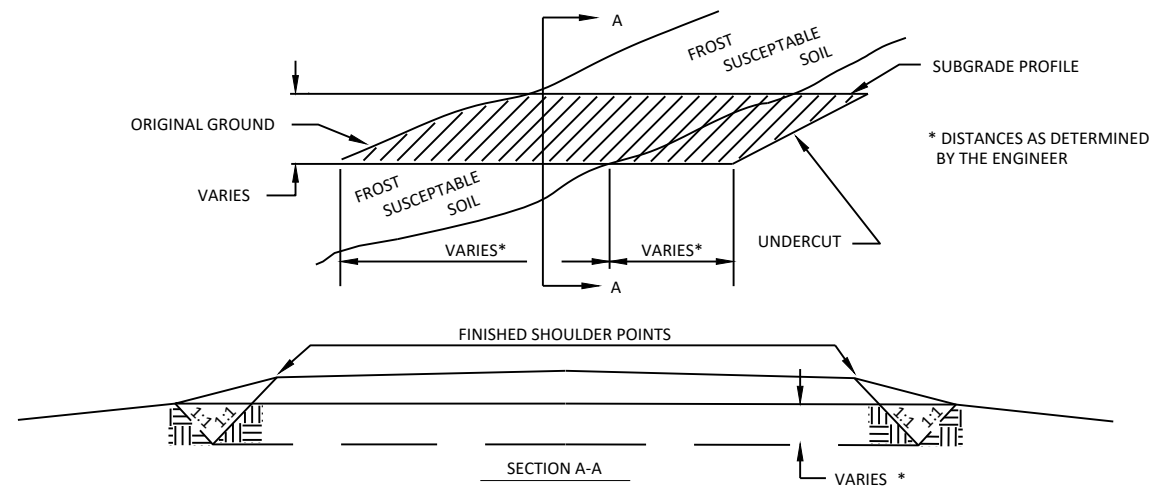
PROFILE VIEW



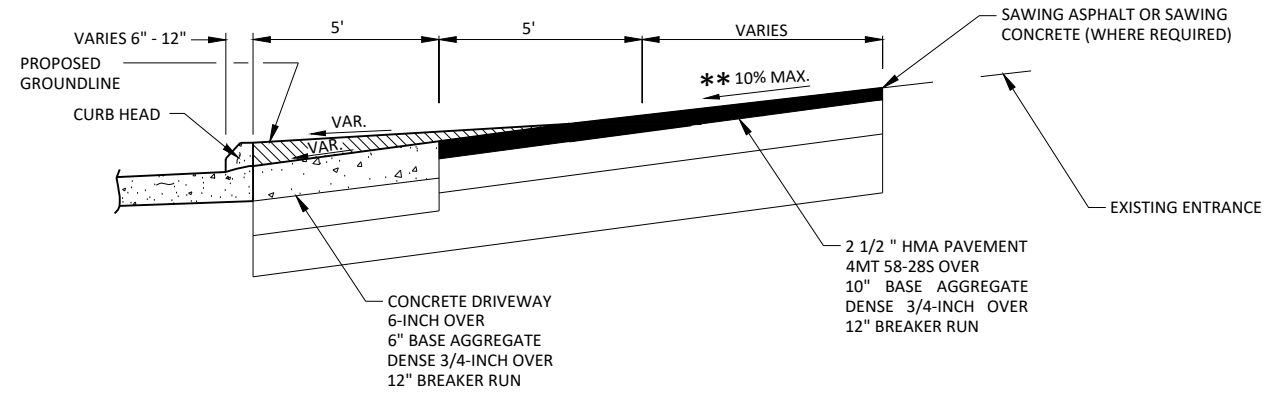
CROSS SECTION VIEW

1. EXACT LOCATION OF E.B.S. (EXCAVATION BELOW SUBGRADE) SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD.
2. E.B.S. AREA TO BE BACKFILLED WITH MATERIAL ACCEPTABLE TO THE ENGINEER. BACKFILL MUST BE HOMOGENEOUS WITH ADJOINING FILL MATERIAL.
3. THE FILL SECTION WITHIN 100' OF THE MOUTH OF THE CUT MUST BE KEPT 2' BELOW SUBGRADE UNTIL E.B.S. IS COMPLETED. LATERAL LIMITS OF EXCAVATION SHALL BE THE SUBGRADE SHOULDER POINTS.

EXCAVATION BELOW SUBGRADE (E.B.S.)

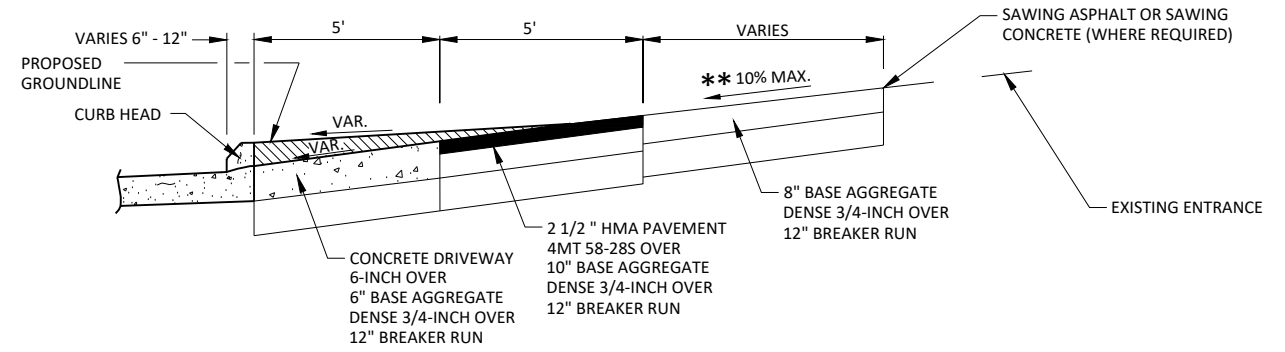


DETAIL FOR EXCAVATION AT MOUTH OF CUTS



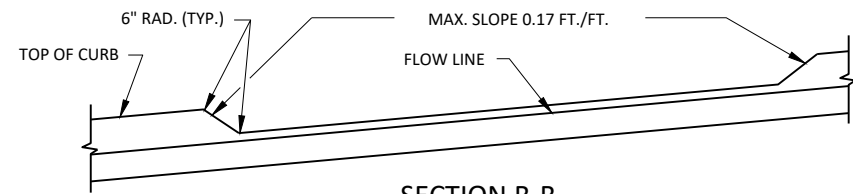
SECTION A-A (HMA)

** 10% MAX UNLESS OTHERWISE NOTED

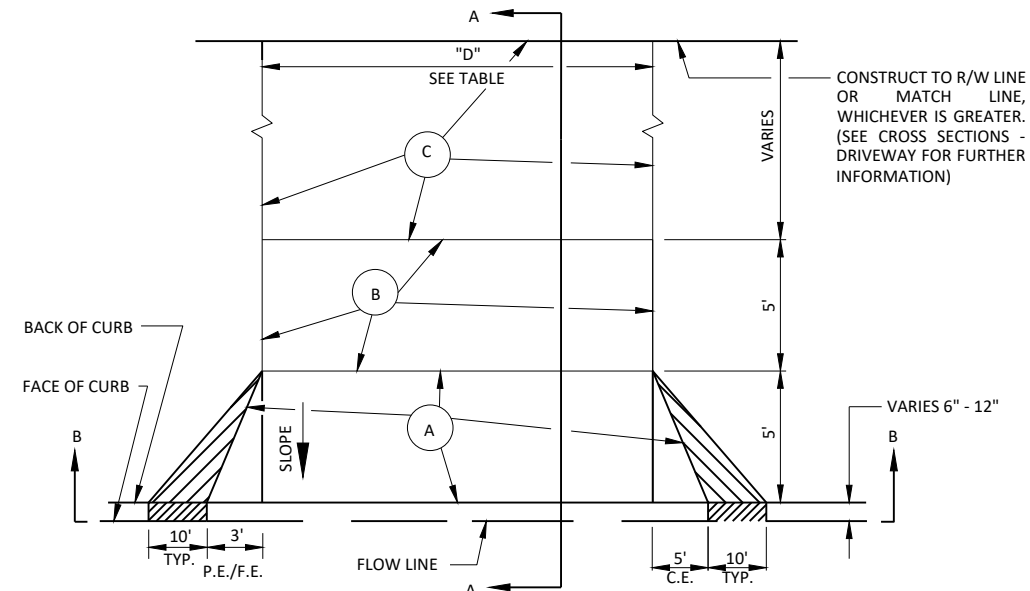


SECTION A-A (B.A.D.)

** 10% MAX UNLESS OTHERWISE NOTED



SECTION B-B

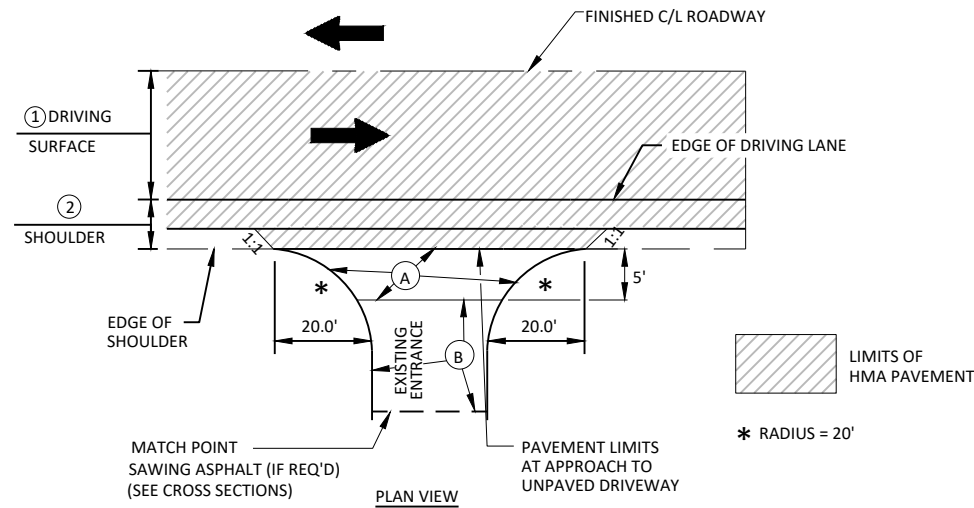


URBAN DRIVEWAY DETAIL

- | | |
|--|--|
| <p>EXISTING</p> <ul style="list-style-type: none"> (A) ALL TYPES (B) ALL TYPES (C) CRUSHED AGGREGATE BASE COURSE ASPHALT | <p>PROPOSED</p> <ul style="list-style-type: none"> CONCRETE DRIVEWAY 6-INCH OVER 6" BASE AGGREGATE DENSE 3/4-INCH OVER 12" BREAKER RUN 2 1/2" HMA PAVEMENT 4MT 58-28S OVER 10" BASE AGGREGATE DENSE 3/4-INCH OVER 12" BREAKER RUN 8" BASE AGGREGATE DENSE 3/4-INCH OVER 12" BREAKER RUN 2 1/2" HMA PAVEMENT 4MT 58-28S OVER 10" BASE AGGREGATE DENSE 3/4-INCH OVER 12" BREAKER RUN |
|--|--|

URBAN DRIVEWAY INFORMATION TABLE

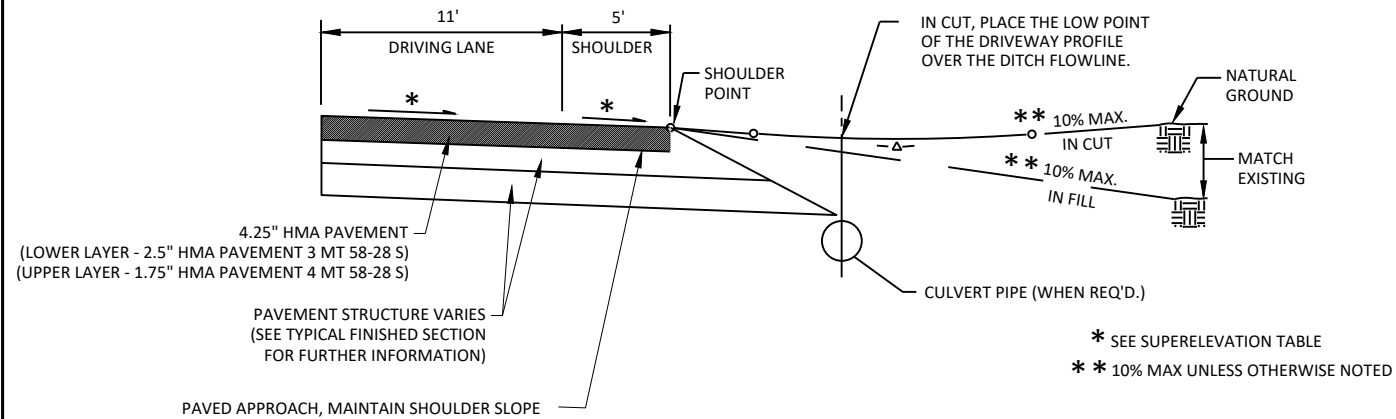
STATION	TYPE	EXISTING PAVEMENT STRUCTURE	PROPOSED PAVEMENT STRUCTURE		"D" WIDTH
			C	"D"	
15+00, LT.	F.E.	N/A	2 1/2" HMA PAVEMENT OVER 10" B.A.D. OVER 12" BREAKER RUN	24	
15+00, RT.	F.E.	N/A	2 1/2" HMA PAVEMENT OVER 10" B.A.D. OVER 12" BREAKER RUN	24	
23+10, LT.	C.E.	N/A	8" B.A.D. OVER 12" BREAKER RUN	22	
26+15, RT.	P.E.	B.A.D	8" B.A.D. OVER 12" BREAKER RUN	36	
26+23, LT.	P.E.	B.A.D	8" B.A.D. OVER 12" BREAKER RUN	20	
27+50, LT. (P'-LINE)	P.E.	B.A.D	2 1/2" HMA PAVEMENT OVER 10" B.A.D. OVER 12" BREAKER RUN	16	
30+52, LT.	F.E.	B.A.D	8" B.A.D. OVER 12" BREAKER RUN	14	
53+58, LT.	P.E.	B.A.D	8" B.A.D. OVER 12" BREAKER RUN	16	
57+74, RT.	F.E.	B.A.D	8" B.A.D. OVER 12" BREAKER RUN	24	
172+04, LT.	P.E.	B.A.D	8" B.A.D. OVER 12" BREAKER RUN	12	
174+15, LT.	F.E.	B.A.D	2 1/2" HMA PAVEMENT OVER 10" B.A.D. OVER 12" BREAKER RUN	16	
260+36, LT.	P.E.	ASPHALT	2 1/2" HMA PAVEMENT OVER 10" B.A.D. OVER 12" BREAKER RUN	14	
261+26, RT.	C.E.	B.A.D	8" B.A.D. OVER 12" BREAKER RUN	24	
400B'+13, LT.	P.E.	ASPHALT	2 1/2" HMA PAVEMENT OVER 10" B.A.D. OVER 12" BREAKER RUN	34	



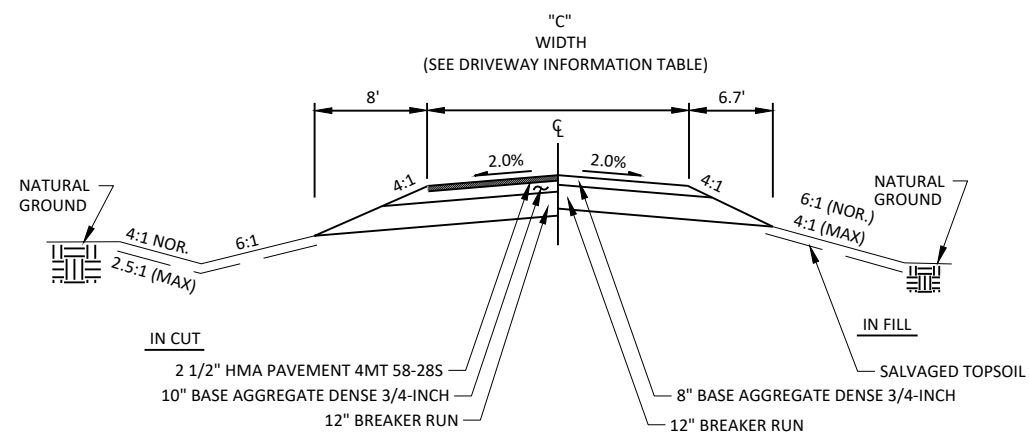
RURAL ENTRANCE DETAIL

ROADWAY	①	②
CTH F	11'	5' (1' PAVED)
'E'-LINE (HORSESHOE BEND ROAD)	10'	2' (0' PAVED)
'H'-LINE (MCKENNA ROAD)	10'	2' (0' PAVED)
'I'-LINE (EAST PECATONICA ROAD)	10'	2' (0' PAVED)

EXISTING	PROPOSED
(A) ALL TYPES	2 1/2" HMA PAVEMENT 4MT 58-28S OVER 10" BASE AGGREGATE DENSE 3/4-INCH OVER 12" BREAKER RUN
(B) CRUSHED AGGREGATE BASE COURSE ASPHALT	8" BASE AGGREGATE DENSE 3/4-INCH OVER 12" BREAKER RUN 2 1/2" HMA PAVEMENT 4MT 58-28S OVER 10" BASE AGGREGATE DENSE 3/4-INCH OVER 12" BREAKER RUN



TYPICAL RURAL ENTRANCE PROFILES

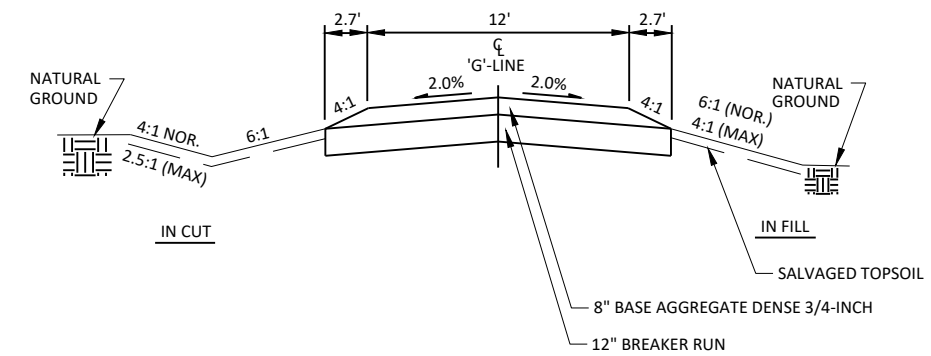


TYPICAL CROSS-SECTION FOR RURAL ENTRANCE

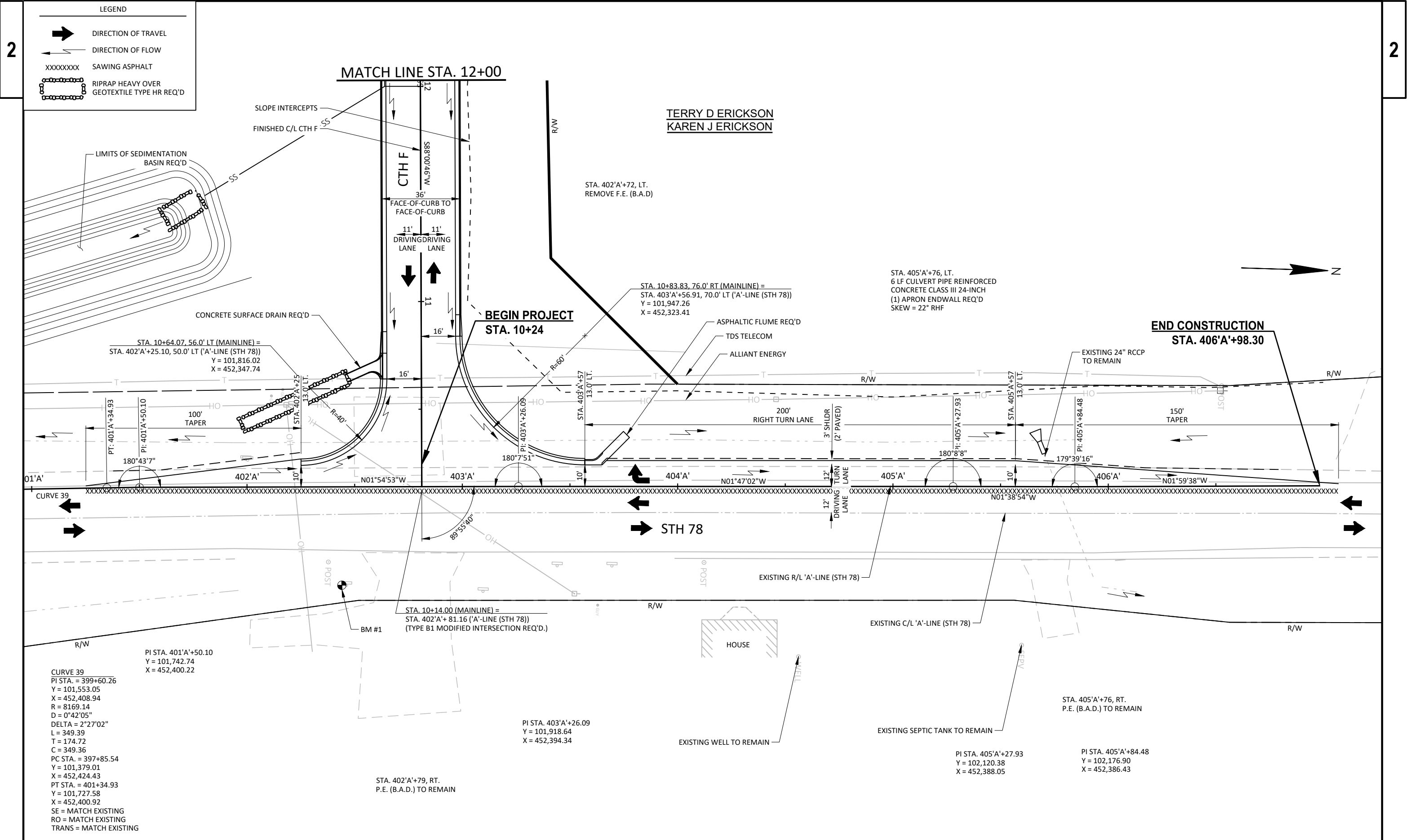
(SEE TYPICAL CROSS SECTION FOR 'G'-LINE)

RURAL DRIVEWAY INFORMATION TABLE

STATION	TYPE	EXISTING PAVEMENT STRUCTURE	PROPOSED PAVEMENT STRUCTURE	'C' WIDTH
34+03, LT. ('D'-LINE)	P.F. (SHARED USE ACCESS)	B.A.D.	8" B.A.D. OVER 12" BREAKER RUN	16
36+50, LT. ('Q'-LINE)	F.E.	B.A.D.	8" B.A.D. OVER 12" BREAKER RUN	16
43+76, LT.	F.F.	B.A.D.	2 1/2" HMA PAVEMENT OVER 10" B.A.D. OVER 12" BREAKER RUN	18
49+92, RT.	F.E.	B.A.D.	2 1/2" HMA PAVEMENT OVER 10" B.A.D. OVER 12" BREAKER RUN	24
50+75, LT.	F.E.	B.A.D.	2 1/2" HMA PAVEMENT OVER 10" B.A.D. OVER 12" BREAKER RUN	20
53+90, LT.	P.F.	B.A.D.	2 1/2" HMA PAVEMENT OVER 10" B.A.D. OVER 12" BREAKER RUN	24
65+25, RT. ('K'-LINE)	P.E.	B.A.D.	2 1/2" HMA PAVEMENT OVER 10" B.A.D. OVER 12" BREAKER RUN	16
73+45, LT.	F.E.	B.A.D.	2 1/2" HMA PAVEMENT OVER 10" B.A.D. OVER 12" BREAKER RUN	16
73+90, LT.	F.E.	B.A.D.	2 1/2" HMA PAVEMENT OVER 10" B.A.D. OVER 12" BREAKER RUN	16
86+20, LT.	F.E.	B.A.D.	8" B.A.D. OVER 12" BREAKER RUN	16
91+43, LT.	P.F.	ASPHALT	2 1/2" HMA PAVEMENT OVER 10" B.A.D. OVER 12" BREAKER RUN	14
97+72, RT.	P.E.	B.A.D.	8" B.A.D. OVER 12" BREAKER RUN	16
106+48, RT.	F.E.	B.A.D.	2 1/2" HMA PAVEMENT OVER 10" B.A.D. OVER 12" BREAKER RUN	16
107+87, LT.	P.E.	B.A.D.	8" B.A.D. OVER 12" BREAKER RUN	16
126+27, LT.	P.E.	B.A.D.	8" B.A.D. OVER 12" BREAKER RUN	16
137+81, LT. ('R'-LINE)	F.F.	B.A.D.	2 1/2" HMA PAVEMENT OVER 10" B.A.D. OVER 12" BREAKER RUN	16
140+39, RT.	F.E.	B.A.D.	2 1/2" HMA PAVEMENT OVER 10" B.A.D. OVER 12" BREAKER RUN	16
142+47, LT.	P.E.	B.A.D.	8" B.A.D. OVER 12" BREAKER RUN	14
151+78, LT.	F.F.	B.A.D.	2 1/2" HMA PAVEMENT OVER 10" B.A.D. OVER 12" BREAKER RUN	20
153+71, RT.	P.E.	ASPHALT	2 1/2" HMA PAVEMENT OVER 10" B.A.D. OVER 12" BREAKER RUN	12
172+79, RT.	P.F.	B.A.D.	8" B.A.D. OVER 12" BREAKER RUN	16
178+43, LT.	F.E.	B.A.D.	2 1/2" HMA PAVEMENT OVER 10" B.A.D. OVER 12" BREAKER RUN	20
193+81, RT.	C.C.	B.A.D.	8" B.A.D. OVER 12" BREAKER RUN	60
195+18, LT.	F.F.	B.A.D.	2 1/2" HMA PAVEMENT OVER 10" B.A.D. OVER 12" BREAKER RUN	18
203+33, RT.	F.E.	B.A.D.	8" B.A.D. OVER 12" BREAKER RUN	16
203+47, LT.	F.C.	B.A.D.	8" B.A.D. OVER 12" BREAKER RUN	16
209+72, RT.	F.E.	B.A.D.	2 1/2" HMA PAVEMENT OVER 10" B.A.D. OVER 12" BREAKER RUN	16
212+27, LT.	F.E.	B.A.D.	2 1/2" HMA PAVEMENT OVER 10" B.A.D. OVER 12" BREAKER RUN	16
212+65, RT.	F.F.	B.A.D.	2 1/2" HMA PAVEMENT OVER 10" B.A.D. OVER 12" BREAKER RUN	16
215+78, RT.	F.E.	B.A.D.	2 1/2" HMA PAVEMENT OVER 10" B.A.D. OVER 12" BREAKER RUN	16
221+75, RT.	F.E.	B.A.D.	8" B.A.D. OVER 12" BREAKER RUN	16
221+82, LT. ('L'-LINE)	P.E.	B.A.D.	8" B.A.D. OVER 12" BREAKER RUN	16
222+27, RT.	F.E.	B.A.D.	8" B.A.D. OVER 12" BREAKER RUN	16
236+52, RT.	F.C.	B.A.D.	2 1/2" HMA PAVEMENT OVER 10" B.A.D. OVER 12" BREAKER RUN	16
236+75, LT. ('M'-LINE)	F.E.	B.A.D.	8" B.A.D. OVER 12" BREAKER RUN	16
247+45, LT.	P.E.	B.A.D.	8" B.A.D. OVER 12" BREAKER RUN	12
251+29, LT.	F.F.	B.A.D.	8" B.A.D. OVER 12" BREAKER RUN	16
254+11, LT.	F.E.	B.A.D.	8" B.A.D. OVER 12" BREAKER RUN	16
257+15, RT.	P.C.	ASPHALT	2 1/2" HMA PAVEMENT OVER 10" B.A.D. OVER 12" BREAKER RUN	16
258+50, RT.	F.E.	B.A.D.	8" B.A.D. OVER 12" BREAKER RUN	16
402'D+40, RT. ('C'-LINE)	P.E.	ASPHALT	2 1/2" HMA PAVEMENT OVER 10" B.A.D. OVER 12" BREAKER RUN	22
401'E+63, LT.	F.C.	B.A.D.	2 1/2" HMA PAVEMENT OVER 10" B.A.D. OVER 12" BREAKER RUN	16
'G'-LINE	P.E.	B.A.D.	8" B.A.D. OVER 12" BREAKER RUN	12
401'F+44, LT.	P.C.	B.A.D.	8" B.A.D. OVER 12" BREAKER RUN	15
402'F+00, RT.	F.E.	B.A.D.	2 1/2" HMA PAVEMENT OVER 10" B.A.D. OVER 12" BREAKER RUN	16



TYPICAL CROSS-SECTION FOR 'G'-LINE



2

2

TERRY D ERICKSON
KAREN J ERICKSON

LEGEND

- DIRECTION OF TRAVEL
- DIRECTION OF FLOW
- XXXXXXX SAWING ASPHALT
- RIPRAP HEAVY OVER GEOTEXTILE TYPE HR REQ'D

CURVE 39
 PI STA. = 399+60.26
 Y = 101,553.05
 X = 452,408.94
 R = 8169.14
 D = 0°42'05"
 DELTA = 2°27'02"
 L = 349.39
 T = 174.72
 C = 349.36
 PC STA. = 397+85.54
 Y = 101,379.01
 X = 452,424.43
 PT STA. = 401+34.93
 Y = 101,727.58
 X = 452,400.92
 SE = MATCH EXISTING
 RO = MATCH EXISTING
 TRANS = MATCH EXISTING

PI STA. 401'A'+50.10
 Y = 101,742.74
 X = 452,400.22

STA. 402'A'+79, RT.
 P.E. (B.A.D.) TO REMAIN

PI STA. 403'A'+26.09
 Y = 101,918.64
 X = 452,394.34

PI STA. 405'A'+27.93
 Y = 102,120.38
 X = 452,388.05

PI STA. 405'A'+84.48
 Y = 102,176.90
 X = 452,386.43

PROJECT NO: 5034-00-72	HWY: CTH F	COUNTY: IOWA	INTERSECTION DETAILS	SHEET	E
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LEGEND

➔ DIRECTION OF TRAVEL

➔ DIRECTION OF FLOW

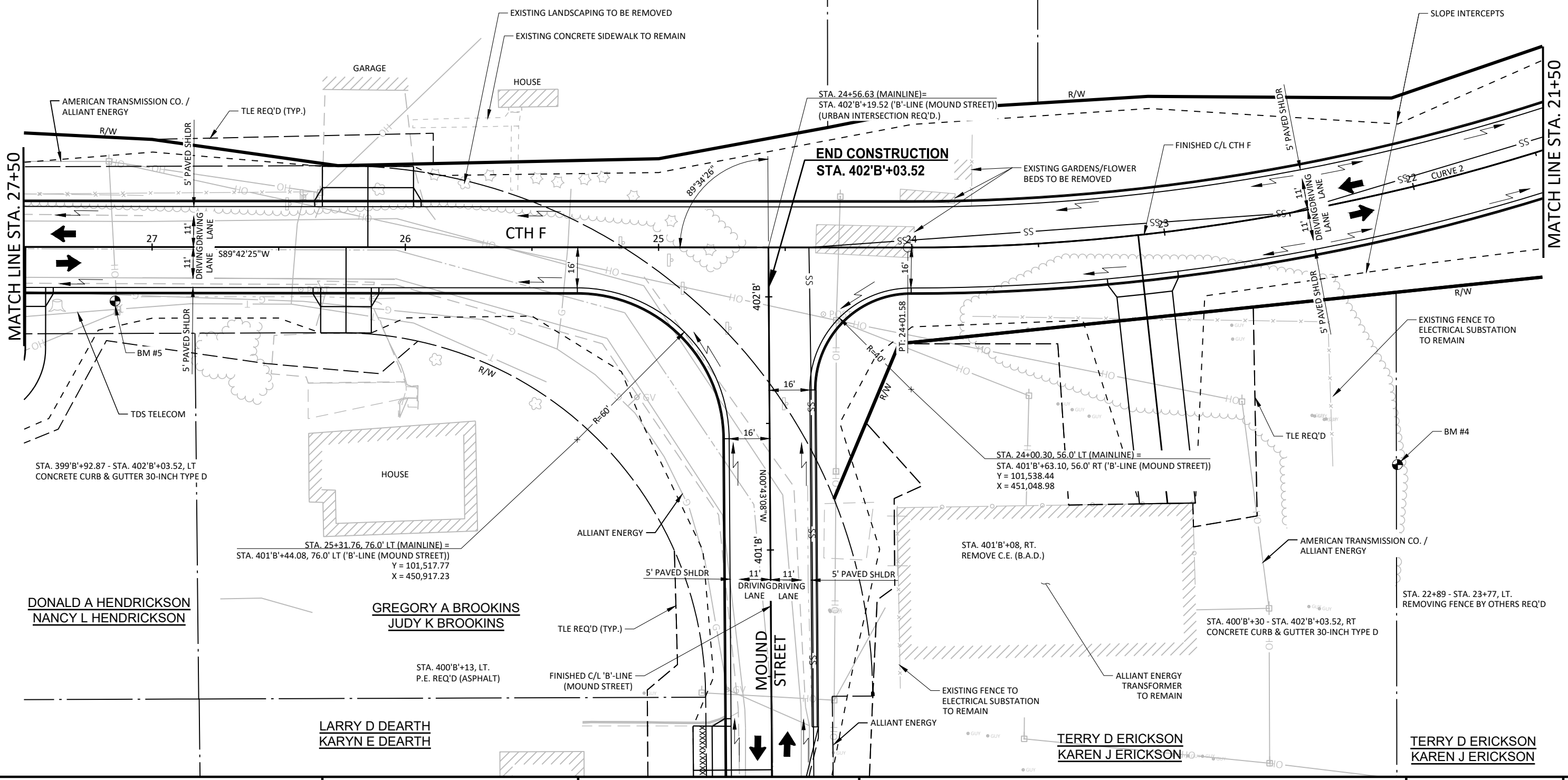
XXXXXXX SAWING ASPHALT



PAULA J SCHUETTE
BRENDA J KITE
SARA J ERICKSON

PAULA J SCHUETTE
BRENDA J KITE
SARA J ERICKSON

TERRY D ERICKSON
KAREN J ERICKSON



PROJECT NO: 5034-00-72

HWY: CTH F

COUNTY: IOWA

INTERSECTION DETAILS

SHEET

E

LEGEND

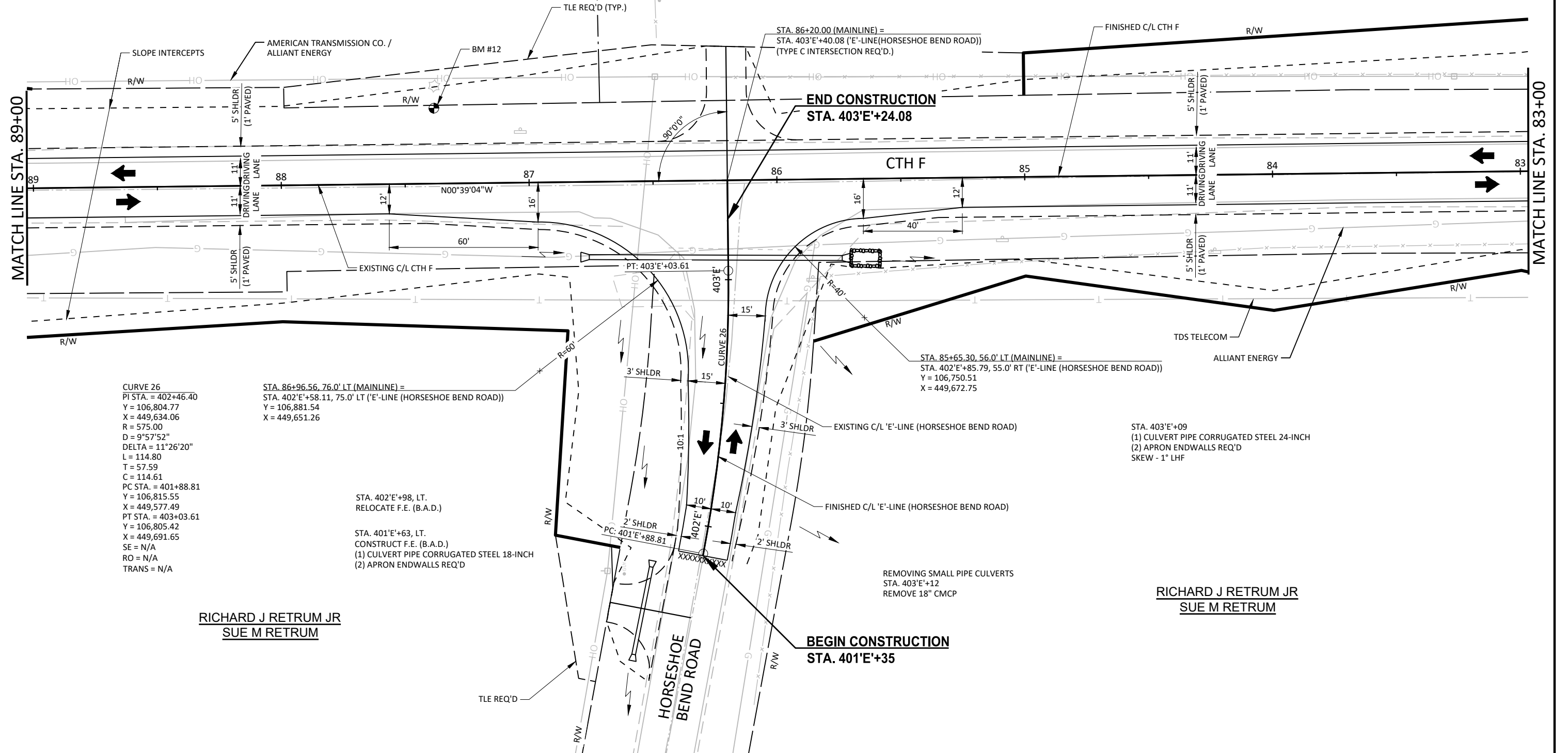
➔ DIRECTION OF TRAVEL

↔ DIRECTION OF FLOW

XXXXXXX SAWING ASPHALT

STEVEN S ZIMMERMAN
STEPHANIE J ZIMMERMAN

RICHARD D ZACHMAN REV TRUST



CURVE 26
 PI STA. = 402+46.40
 Y = 106,804.77
 X = 449,634.06
 R = 575.00
 D = 9°57'52"
 DELTA = 11°26'20"
 L = 114.80
 T = 57.59
 C = 114.61
 PC STA. = 401+88.81
 Y = 106,815.55
 X = 449,577.49
 PT STA. = 403+03.61
 Y = 106,805.42
 X = 449,691.65
 SE = N/A
 RO = N/A
 TRANS = N/A

STA. 86+96.56, 76.0' LT (MAINLINE) =
 STA. 402'E+58.11, 75.0' LT ('E'-LINE (HORSESHOE BEND ROAD))
 Y = 106,881.54
 X = 449,651.26

STA. 402'E+98, LT.
 RELOCATE F.E. (B.A.D.)

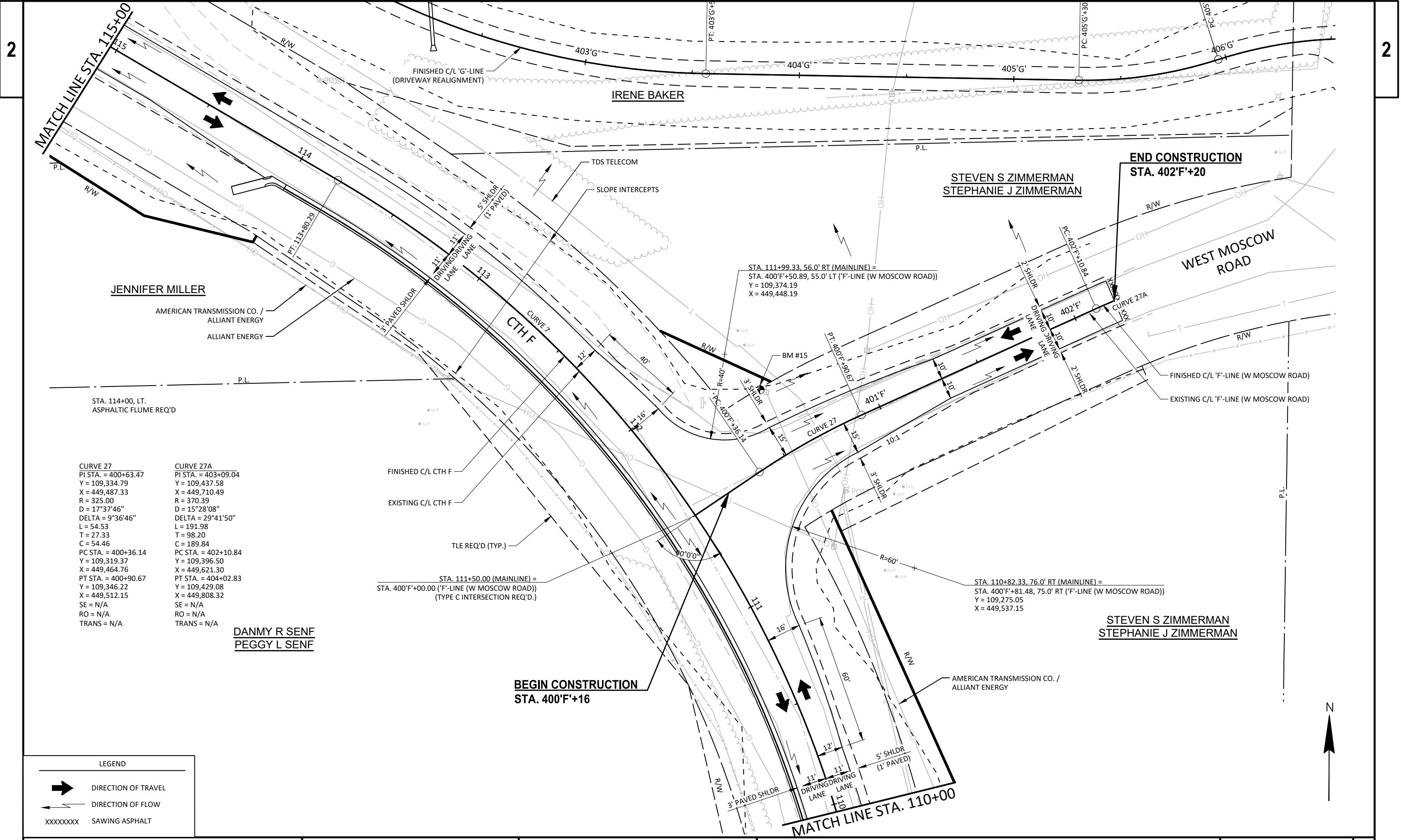
STA. 401'E+63, LT.
 CONSTRUCT F.E. (B.A.D.)
 (1) CULVERT PIPE CORRUGATED STEEL 18-INCH
 (2) APRON ENDWALLS REQ'D

STA. 85+65.30, 56.0' LT (MAINLINE) =
 STA. 402'E+85.79, 55.0' RT ('E'-LINE (HORSESHOE BEND ROAD))
 Y = 106,750.51
 X = 449,672.75

STA. 403'E+09
 (1) CULVERT PIPE CORRUGATED STEEL 24-INCH
 (2) APRON ENDWALLS REQ'D
 SKEW - 1° LHF

REMOVING SMALL PIPE CULVERTS
 STA. 403'E+12
 REMOVE 18" CMCP

RICHARD J RETRUM JR
SUE M RETRUM



2

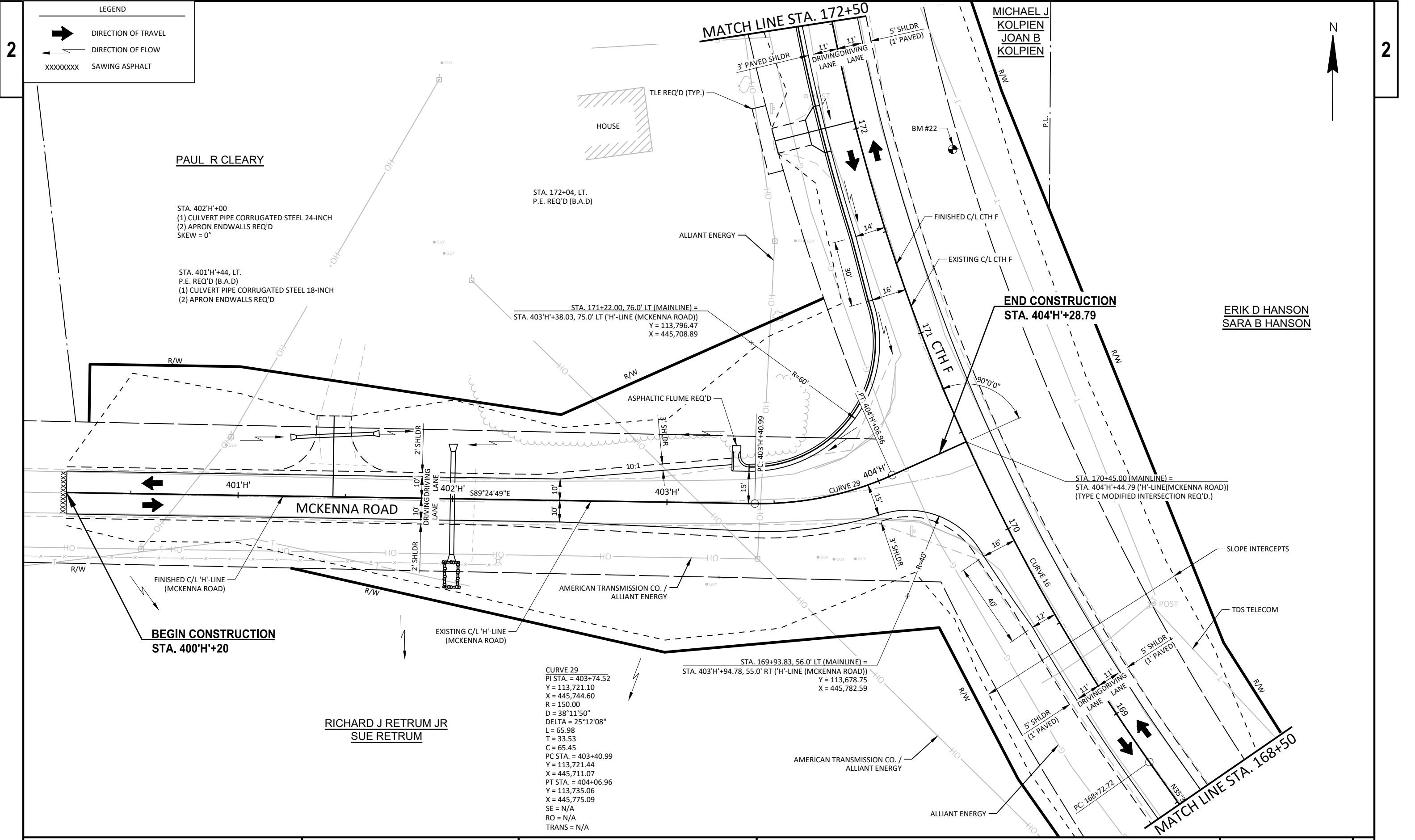
2

CURVE 27	CURVE 27A
PI STA. = 400+63.47	PI STA. = 403+09.04
Y = 109,334.79	Y = 109,437.58
X = 449,487.33	X = 449,710.49
R = 325.00	R = 370.39
D = 17°37'46"	D = 15°28'08"
DELTA = 9°36'46"	DELTA = 29°41'50"
L = 54.53	L = 191.98
T = 27.33	T = 98.20
C = 54.46	C = 189.84
PC STA. = 400+36.14	PC STA. = 402+10.84
Y = 109,319.37	Y = 109,396.50
X = 449,464.76	X = 449,621.30
PT STA. = 400+90.67	PT STA. = 404+02.83
Y = 109,346.22	Y = 109,429.08
X = 449,512.15	X = 449,808.32
SE = N/A	SE = N/A
RO = N/A	RO = N/A
TRANS = N/A	TRANS = N/A

DANMY R SENF
PEGGY L SENF

STEVEN S ZIMMERMAN
STEPHANIE J ZIMMERMAN

LEGEND	
	DIRECTION OF TRAVEL
	DIRECTION OF FLOW
XXXXXXX	SAWING ASPHALT



2

2



LEGEND

➔ DIRECTION OF TRAVEL

➔ DIRECTION OF FLOW

XXXXXXX SAWING ASPHALT

PAUL R. CLEARY

STA. 402'H'+00
 (1) CULVERT PIPE CORRUGATED STEEL 24-INCH
 (2) APRON ENDWALLS REQ'D
 SKEW = 0°

STA. 401'H'+44, LT.
 P.E. REQ'D (B.A.D)
 (1) CULVERT PIPE CORRUGATED STEEL 18-INCH
 (2) APRON ENDWALLS REQ'D

STA. 172+04, LT.
 P.E. REQ'D (B.A.D)

STA. 171+22.00, 76.0' LT (MAINLINE) =
 STA. 403'H'+38.03, 75.0' LT ('H'-LINE (MCKENNA ROAD))
 Y = 113,796.47
 X = 445,708.89

MICHAEL J. KOLPIEN
JOAN B. KOLPIEN

END CONSTRUCTION
STA. 404'H'+28.79

ERIK D. HANSON
SARA B. HANSON

BEGIN CONSTRUCTION
STA. 400'H'+20

RICHARD J. RETRUM JR.
SUE RETRUM

CURVE 29
 PI STA. = 403+74.52
 Y = 113,721.10
 X = 445,744.60
 R = 150.00
 D = 38°11'50"
 DELTA = 25°12'08"
 L = 65.98
 T = 33.53
 C = 65.45
 PC STA. = 403+40.99
 Y = 113,721.44
 X = 445,711.07
 PT STA. = 404+06.96
 Y = 113,735.06
 X = 445,775.09
 SE = N/A
 RO = N/A
 TRANS = N/A

STA. 169+93.83, 56.0' LT (MAINLINE) =
 STA. 403'H'+94.78, 55.0' RT ('H'-LINE (MCKENNA ROAD))
 Y = 113,678.75
 X = 445,782.59

STA. 170+45.00 (MAINLINE) =
 STA. 404'H'+44.79 ('H'-LINE (MCKENNA ROAD))
 (TYPE C MODIFIED INTERSECTION REQ'D.)

PROJECT NO: 5034-00-72	HWY: CTH F	COUNTY: IOWA	INTERSECTION DETAILS	SHEET	E
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LEGEND

➔ DIRECTION OF TRAVEL

➔ DIRECTION OF FLOW

JAMES MCGHEE
MALA MCGHEE

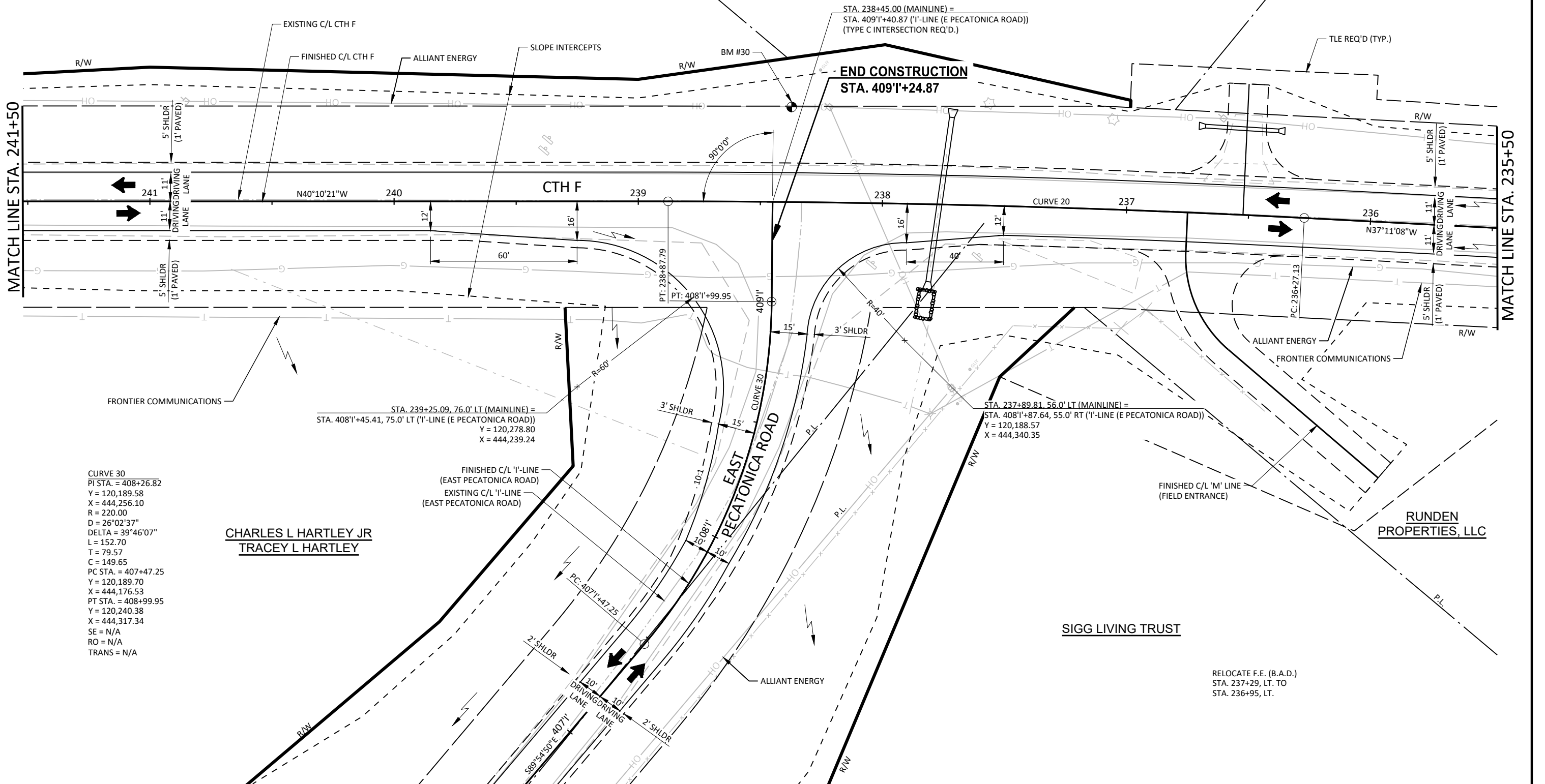
JAMES MCGHEE
MALA MCGHEE

RUNDEN
PROPERTIES, LLC

STA. 236+52, RT.
F.E. REQ'D (B.A.D.)

REMOVING SMALL PIPE CULVERTS
STA. 236+52, RT.
REMOVE 18" CMCP

STA. 236+52, RT.
(1) CULVERT PIPE CLASS III-A 18-INCH
(2) APRON ENDWALLS REQ'D



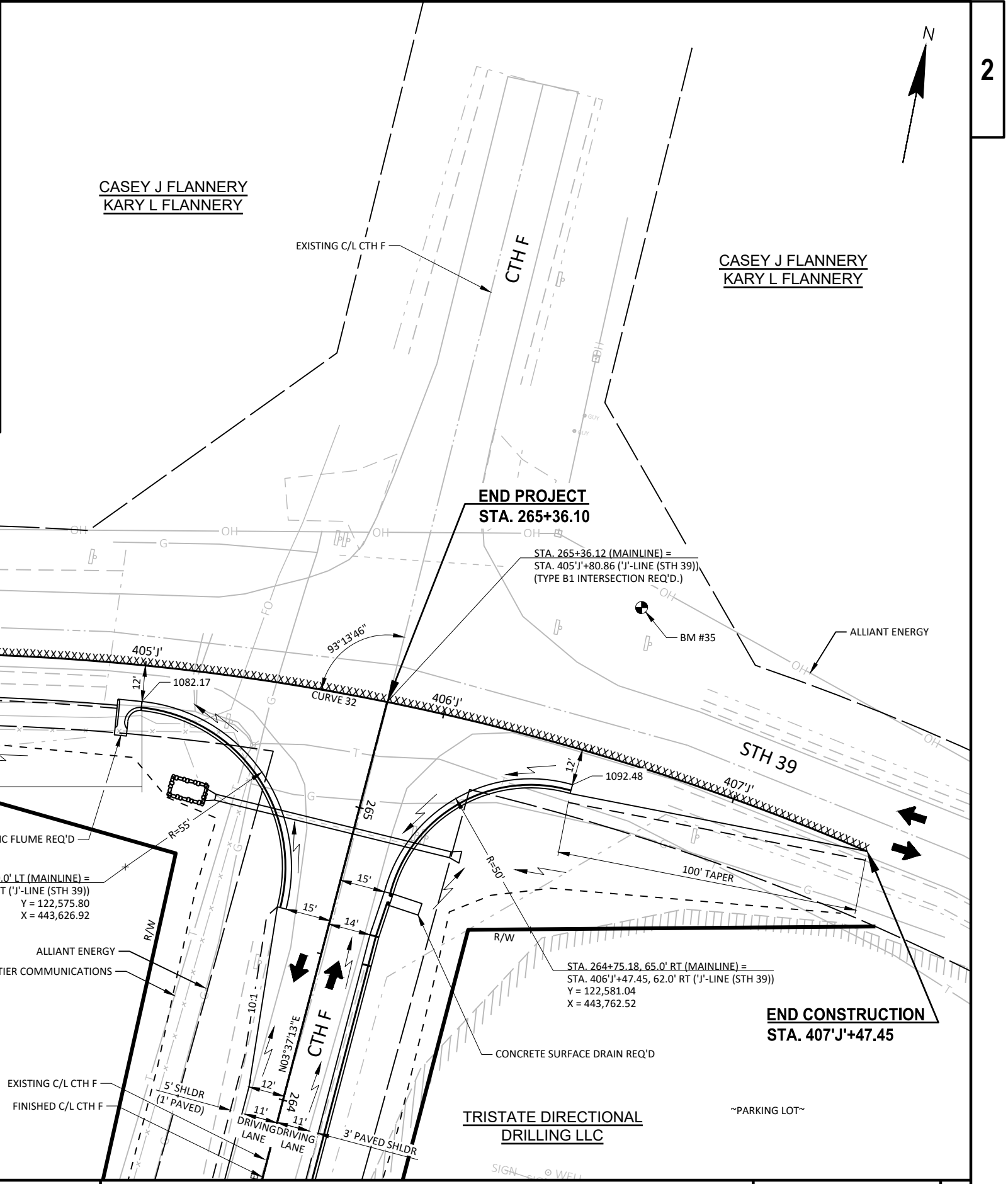
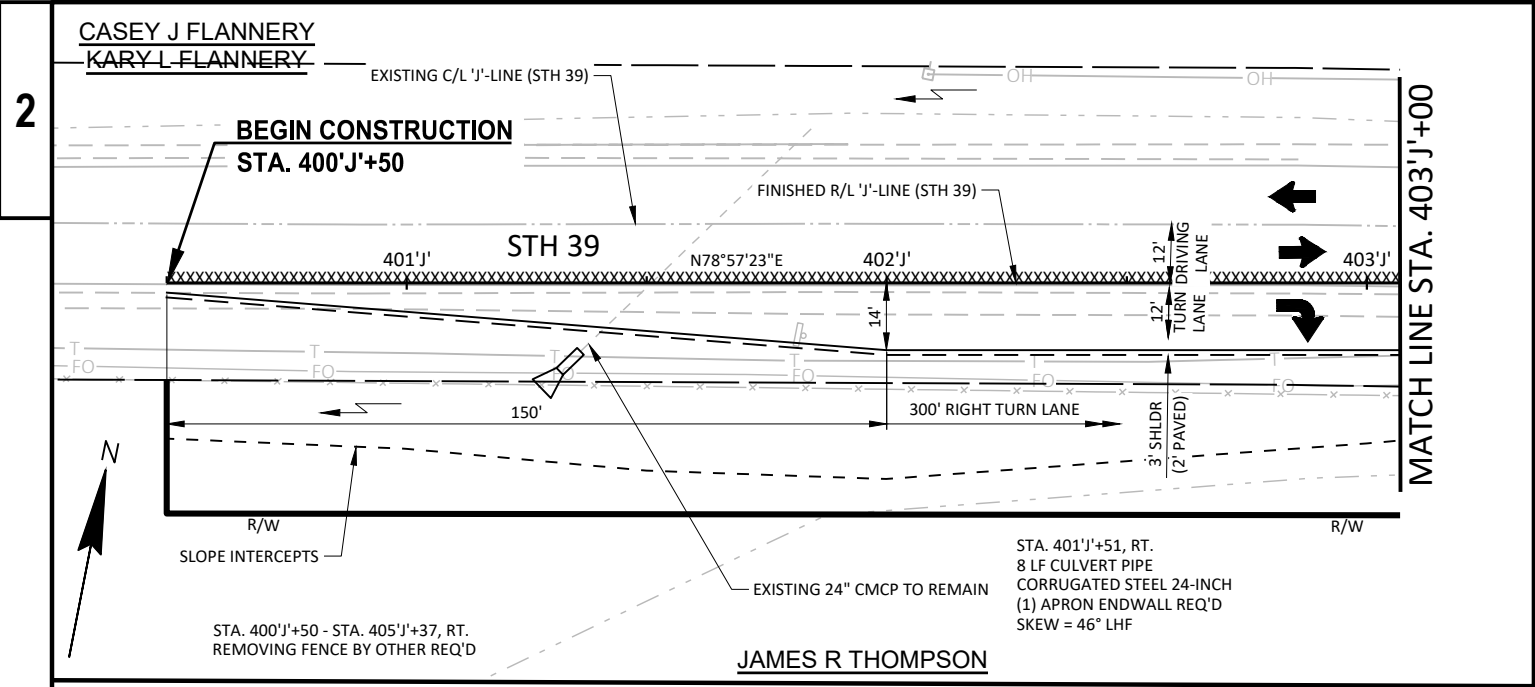
CURVE 30
PI STA. = 408+26.82
Y = 120,189.58
X = 444,256.10
R = 220.00
D = 26°02'37"
DELTA = 39°46'07"
L = 152.70
T = 79.57
C = 149.65
PC STA. = 407+47.25
Y = 120,189.70
X = 444,176.53
PT STA. = 408+99.95
Y = 120,240.38
X = 444,317.34
SE = N/A
RO = N/A
TRANS = N/A

CHARLES L HARTLEY JR
TRACEY L HARTLEY

SIGG LIVING TRUST

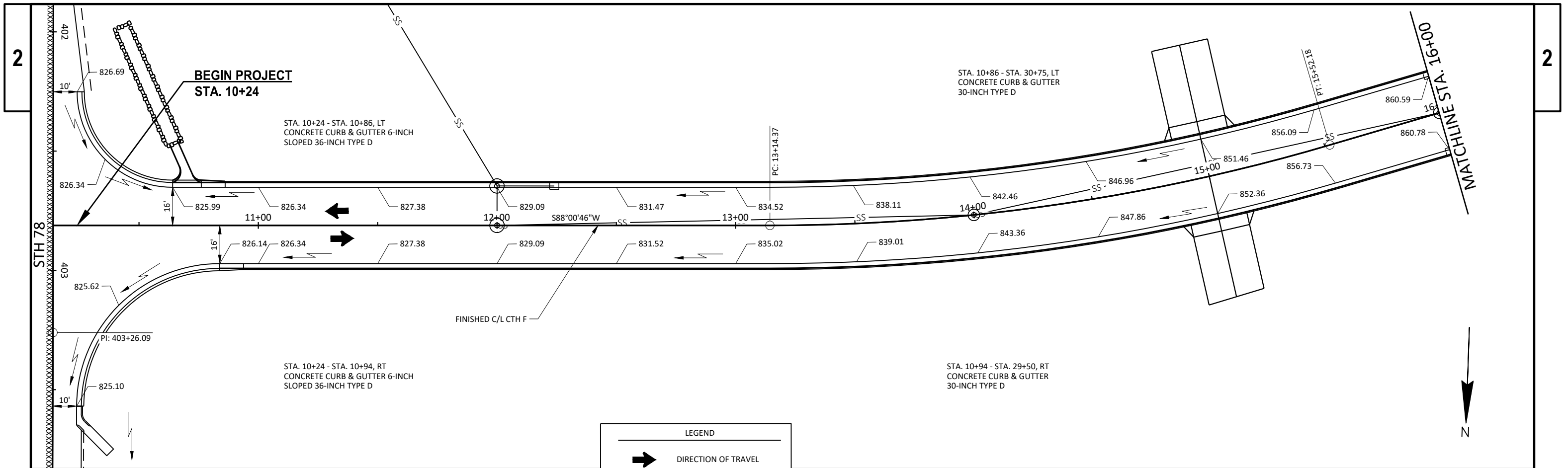
RUNDEN
PROPERTIES, LLC

RELOCATE F.E. (B.A.D.)
STA. 237+29, LT. TO
STA. 236+95, LT.



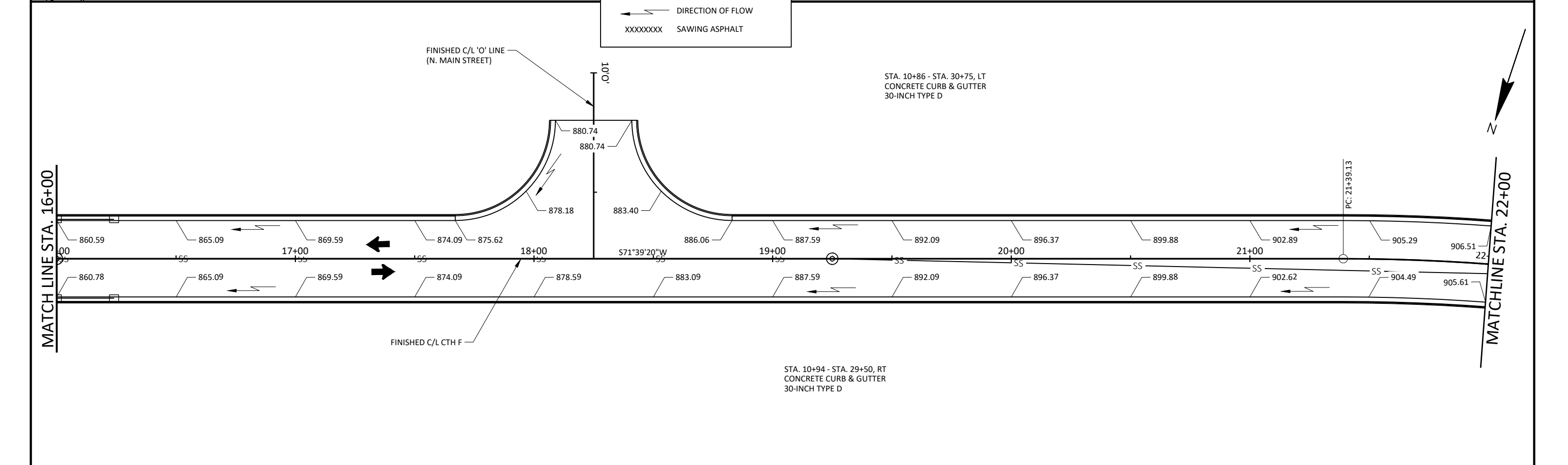
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	DIRECTION OF TRAVEL
	DIRECTION OF FLOW
	SAWING ASPHALT
	RIPRAP HEAVY OVER GEOTEXTILE TYPE HR REQ'D





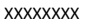
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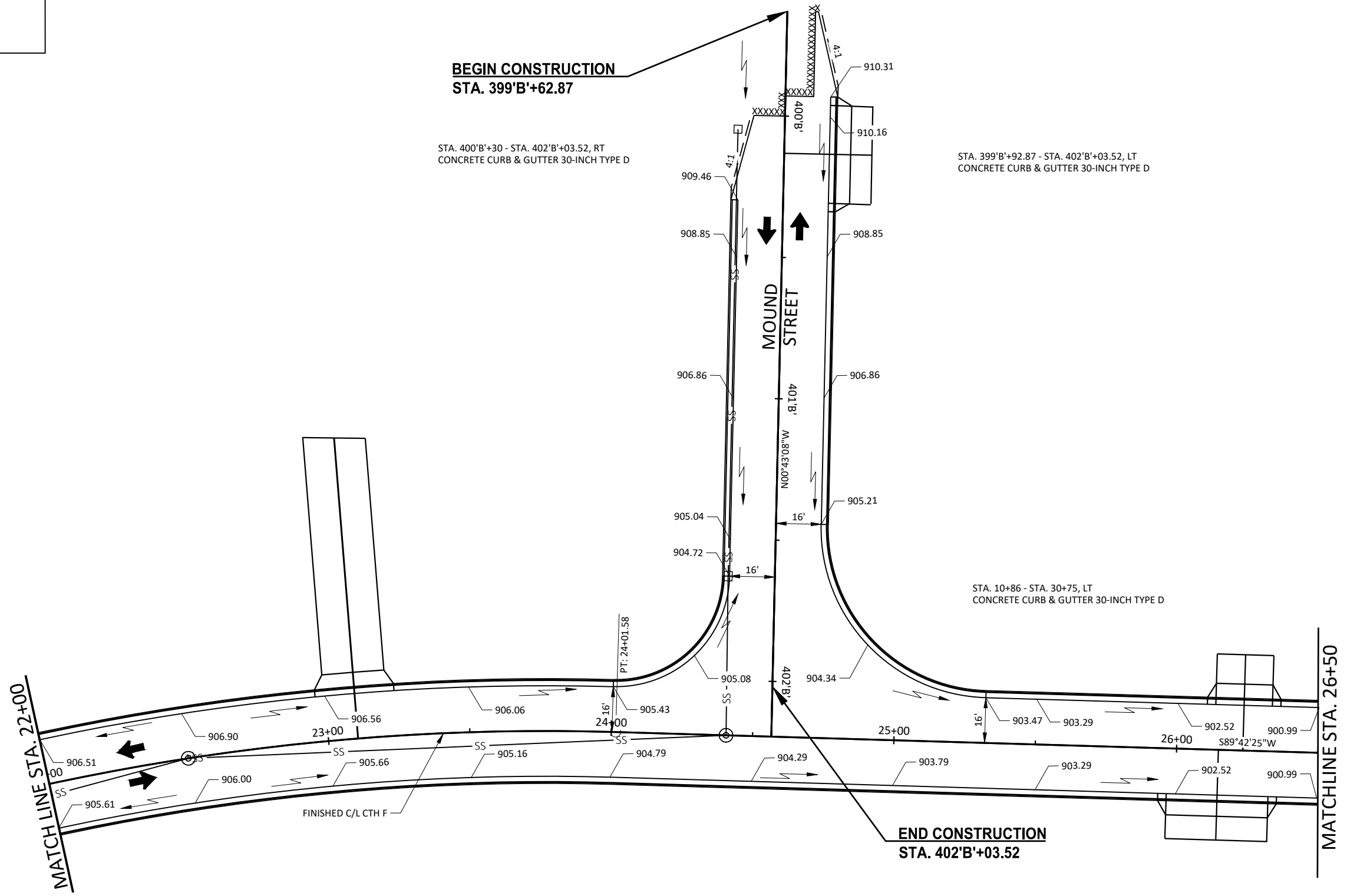
- DIRECTION OF TRAVEL
- DIRECTION OF FLOW
- SAWING ASPHALT



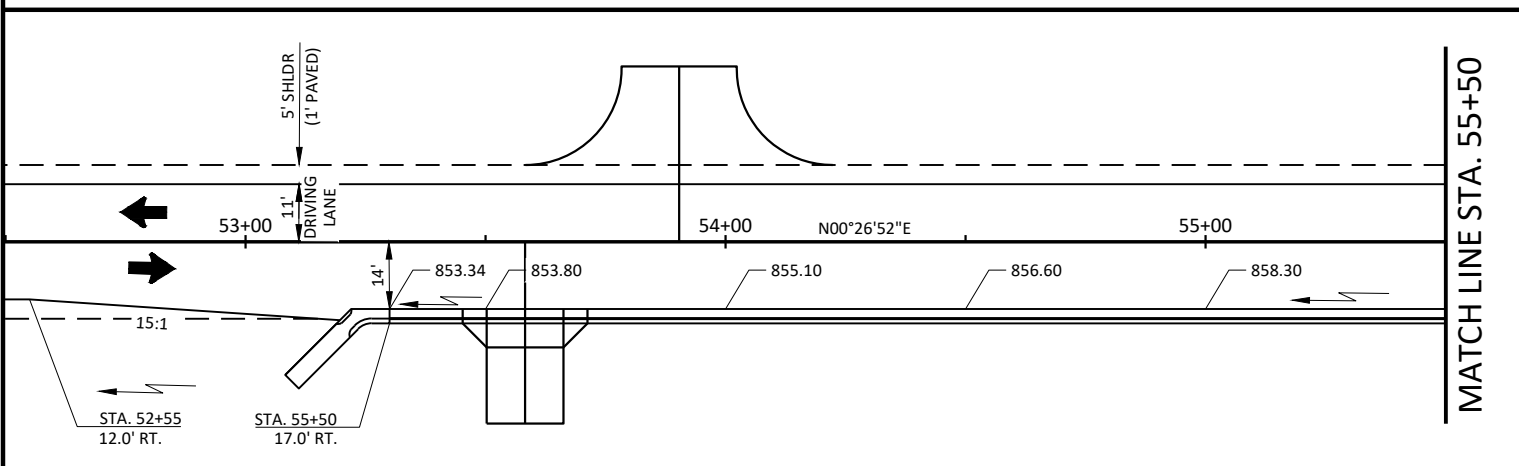
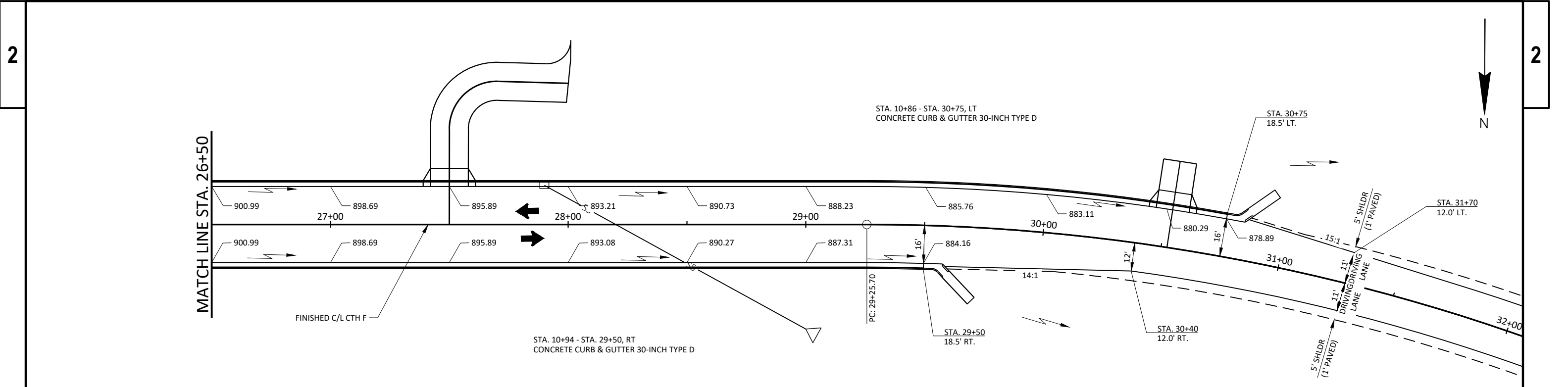
PROJECT NO: 5034-00-72	HWY: CTH F	COUNTY: IOWA	PAVING DETAILS	SHEET	E
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LEGEND

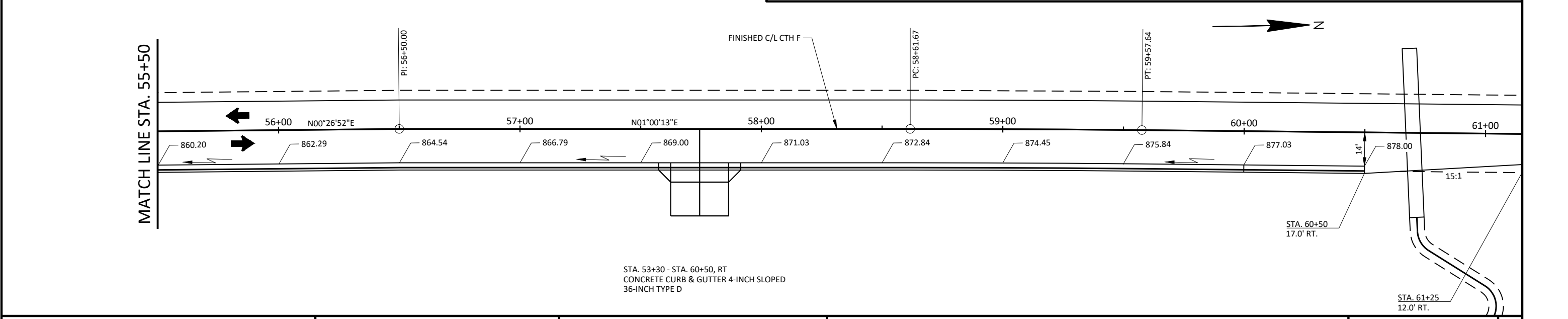
-  DIRECTION OF TRAVEL
-  DIRECTION OF FLOW
-  SAWING ASPHALT



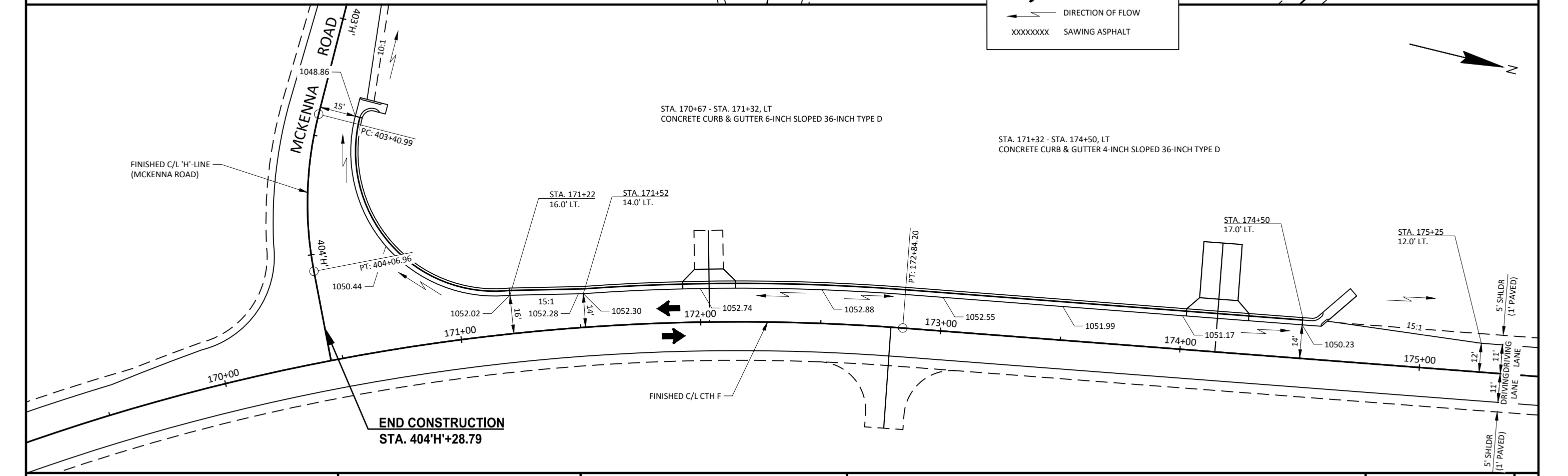
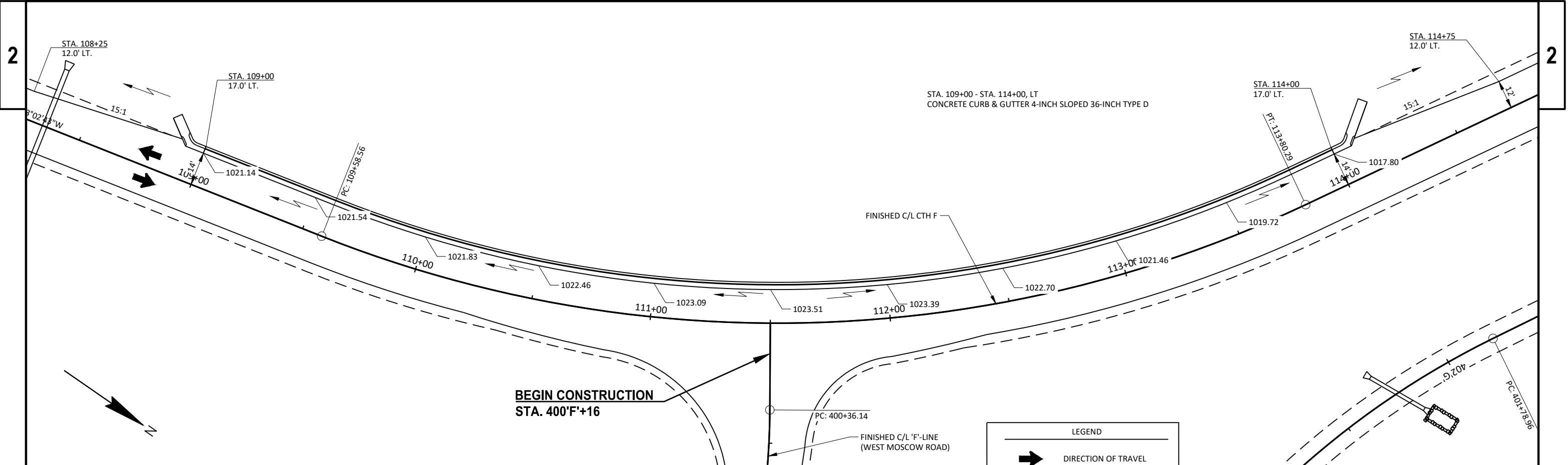
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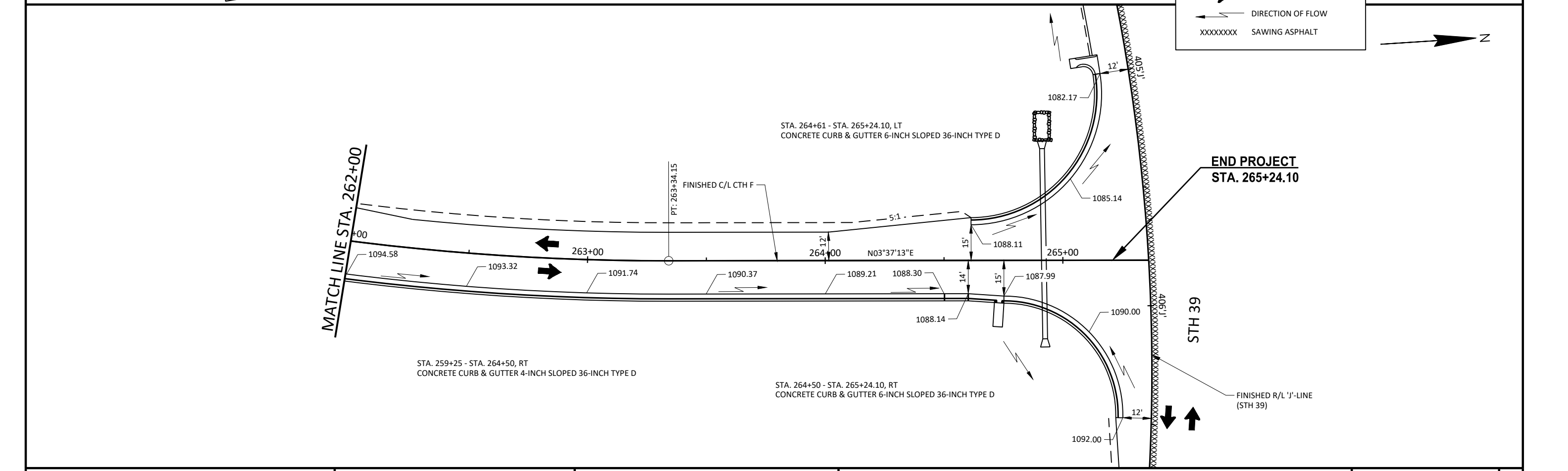
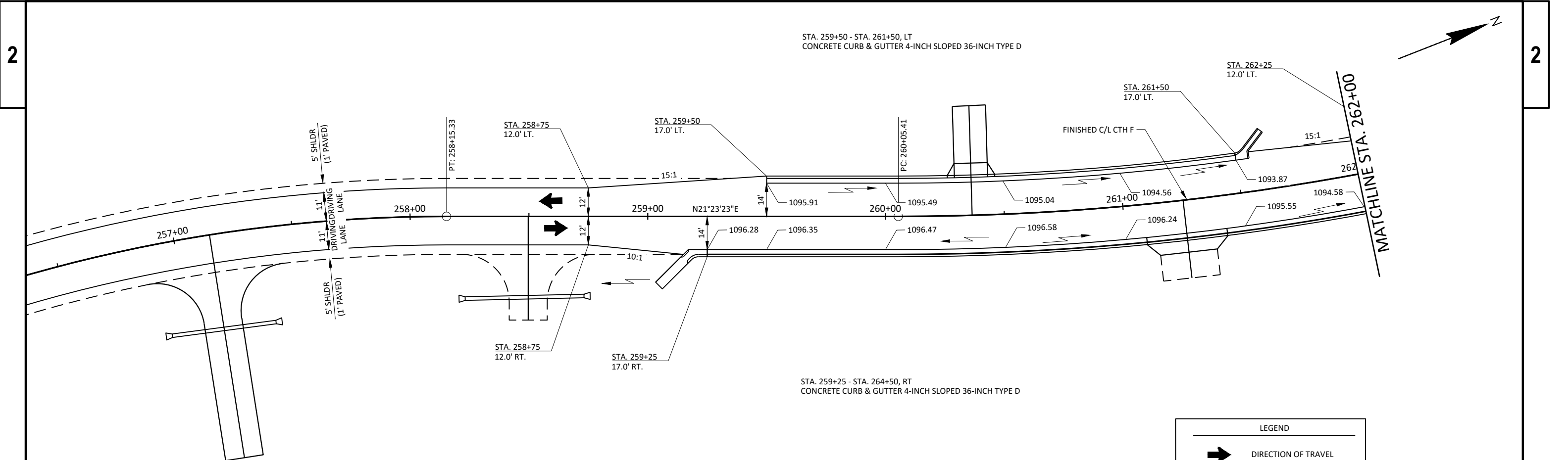
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	DIRECTION OF TRAVEL
	DIRECTION OF FLOW
XXXXXXX	SAWING ASPHALT



PROJECT NO: 5034-00-72	HWY: CTH F	COUNTY: IOWA	PAVING DETAILS	SHEET	E
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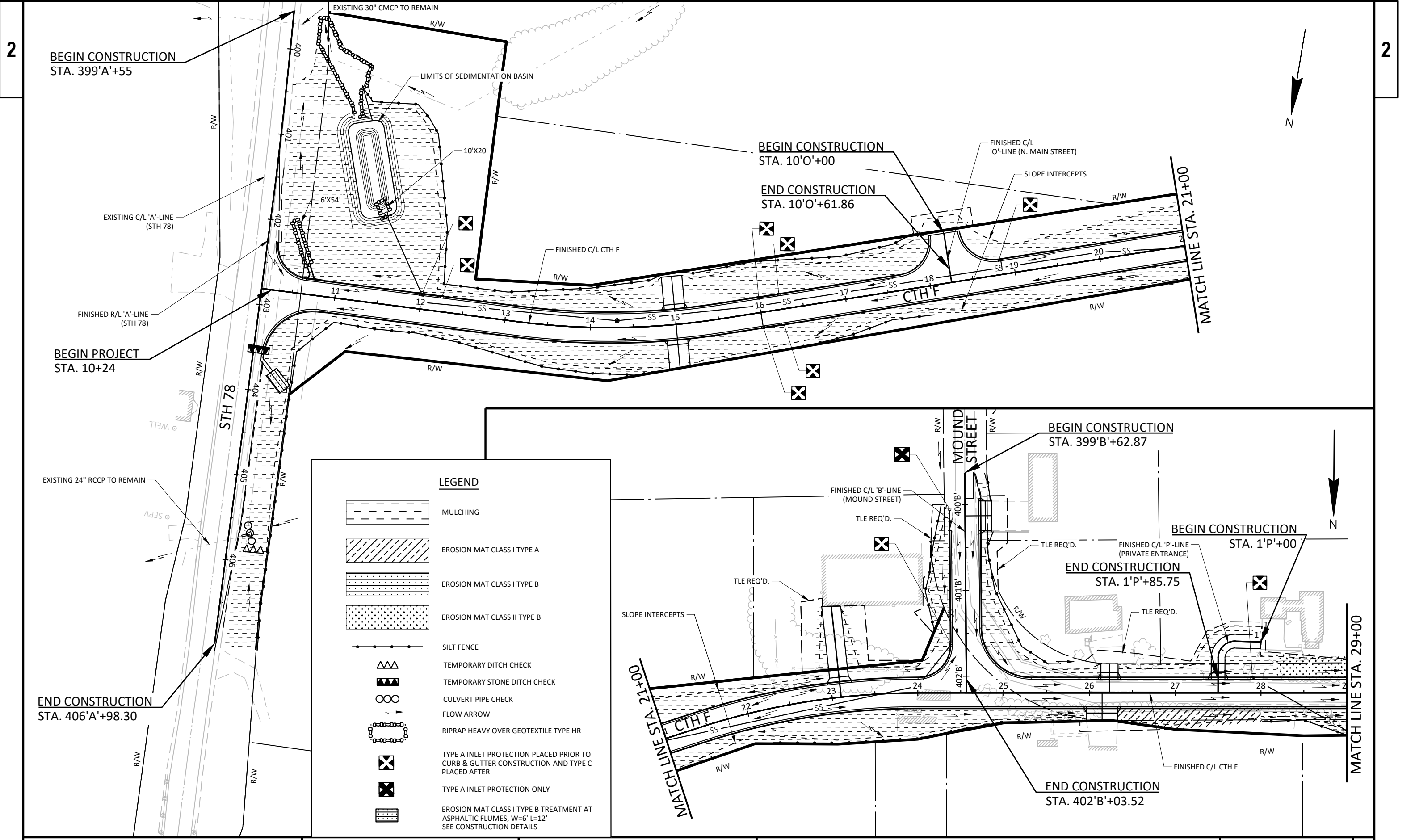
PROJECT NO: 5034-00-72	HWY: CTH F	COUNTY: IOWA	PAVING DETAILS	SHEET	E
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LEGEND

- DIRECTION OF TRAVEL
- DIRECTION OF FLOW
- SAWING ASPHALT

PROJECT NO: 5034-00-72	HWY: CTH F	COUNTY: IOWA	PAVING DETAILS	SHEET	E
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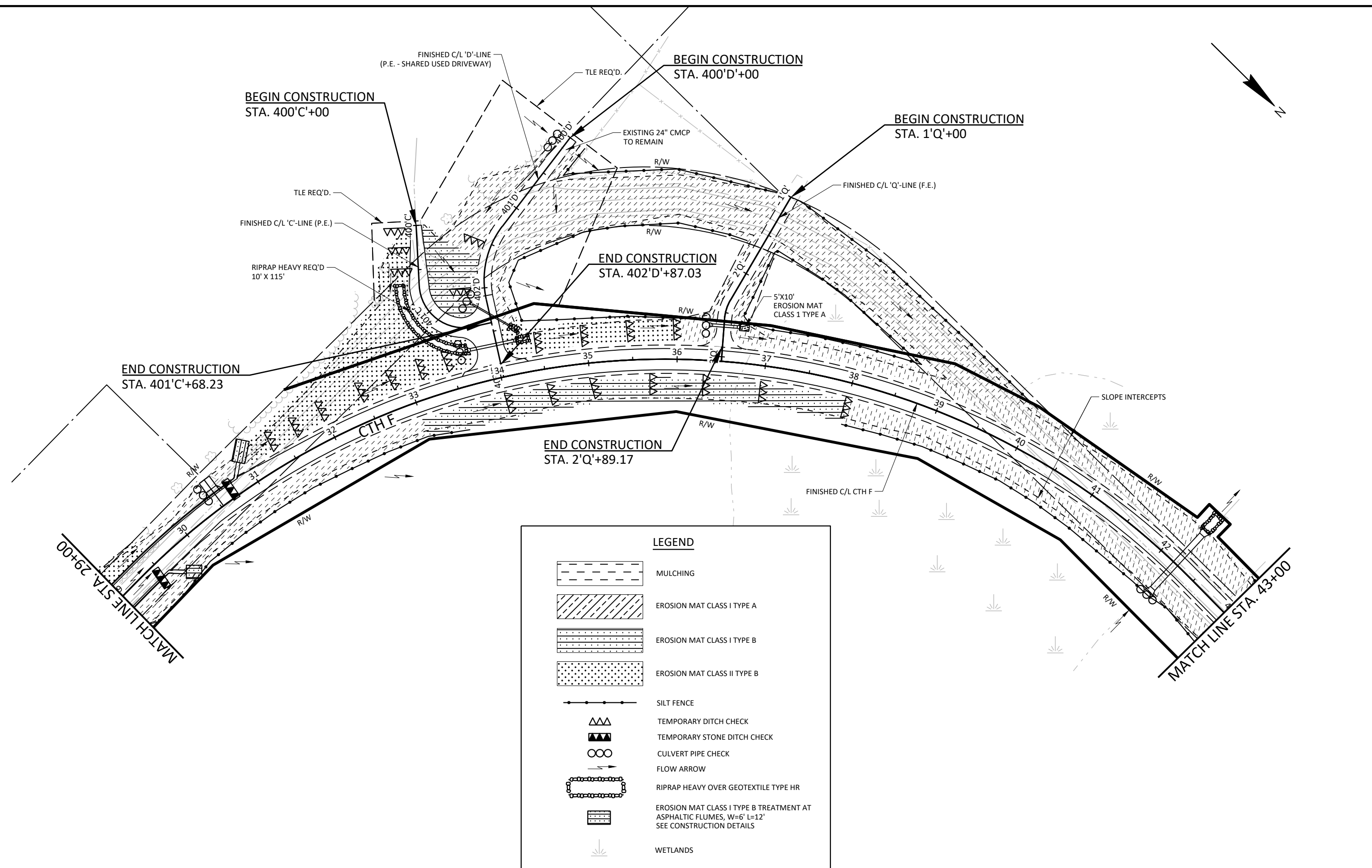
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2



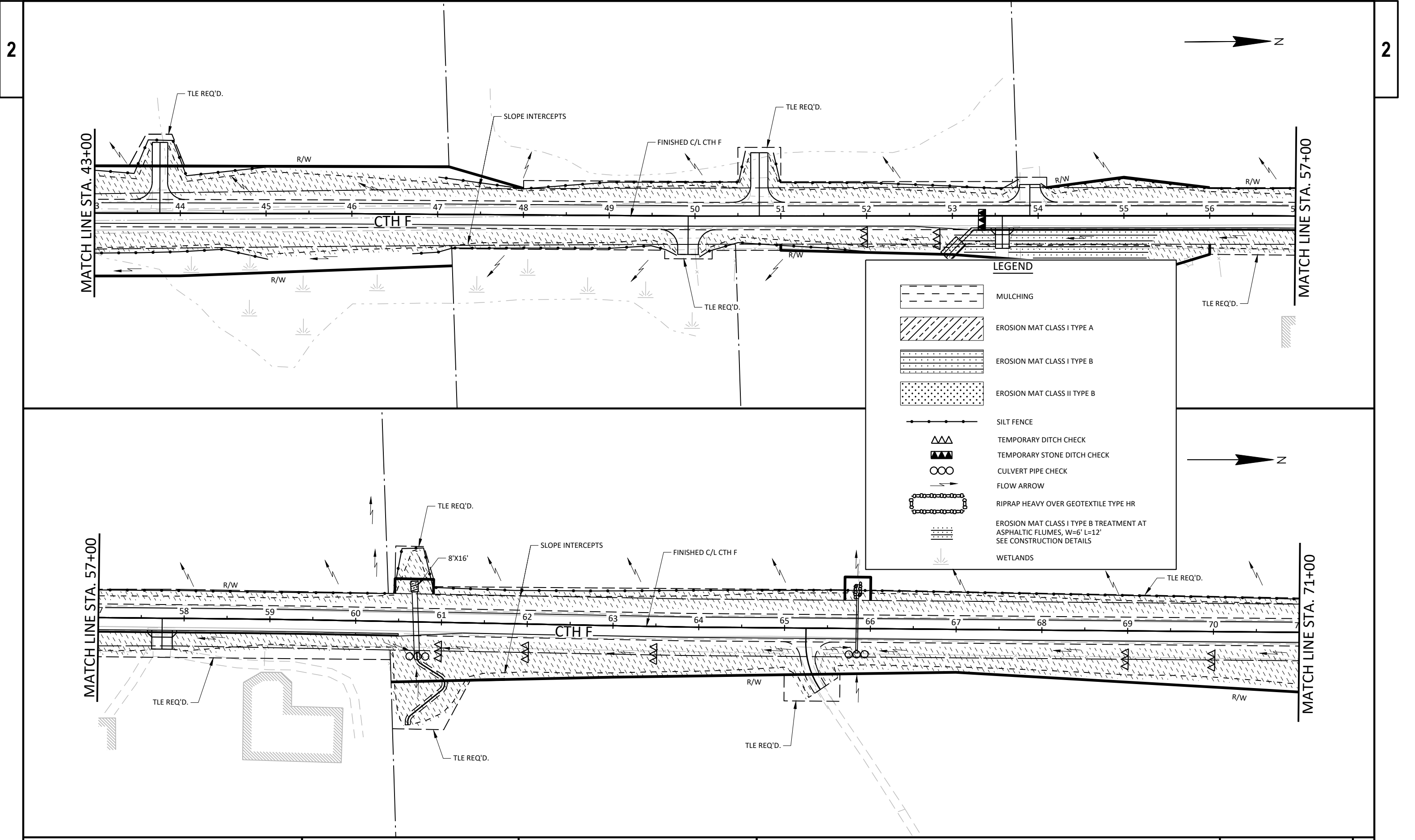
LEGEND

	MULCHING
	EROSION MAT CLASS I TYPE A
	EROSION MAT CLASS I TYPE B
	EROSION MAT CLASS II TYPE B
	SILT FENCE
	TEMPORARY DITCH CHECK
	TEMPORARY STONE DITCH CHECK
	CULVERT PIPE CHECK
	FLOW ARROW
	RIPRAP HEAVY OVER GEOTEXTILE TYPE HR
	TYPE A INLET PROTECTION PLACED PRIOR TO CURB & GUTTER CONSTRUCTION AND TYPE C PLACED AFTER
	TYPE A INLET PROTECTION ONLY
	EROSION MAT CLASS I TYPE B TREATMENT AT ASPHALTIC FLUMES, W=6' L=12' SEE CONSTRUCTION DETAILS



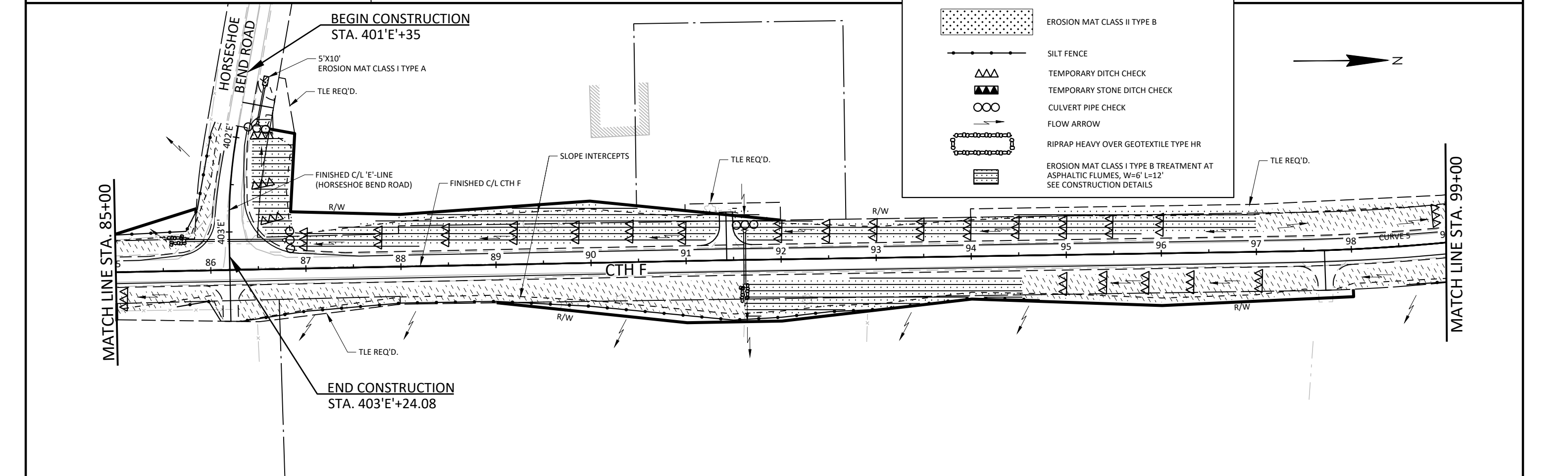
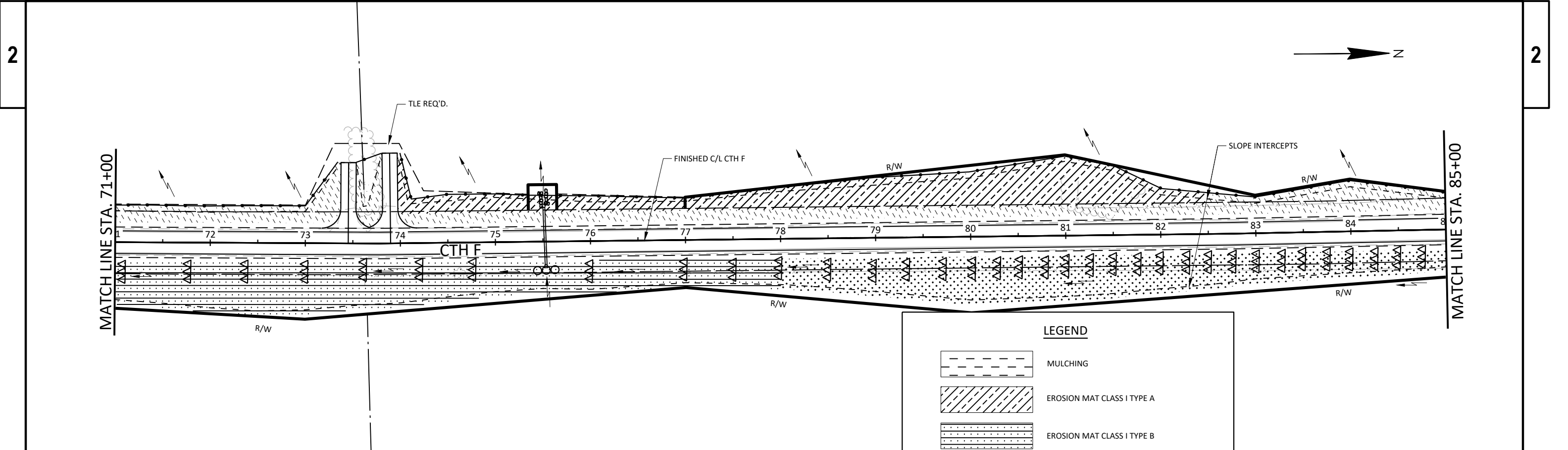
LEGEND

	MULCHING
	EROSION MAT CLASS I TYPE A
	EROSION MAT CLASS I TYPE B
	EROSION MAT CLASS II TYPE B
	SILT FENCE
	TEMPORARY DITCH CHECK
	TEMPORARY STONE DITCH CHECK
	CULVERT PIPE CHECK
	FLOW ARROW
	RIPRAP HEAVY OVER GEOTEXTILE TYPE HR
	EROSION MAT CLASS I TYPE B TREATMENT AT ASPHALTIC FLUMES, W=6' L=12' SEE CONSTRUCTION DETAILS
	WETLANDS

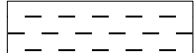
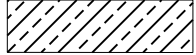
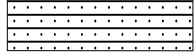
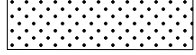
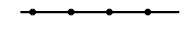








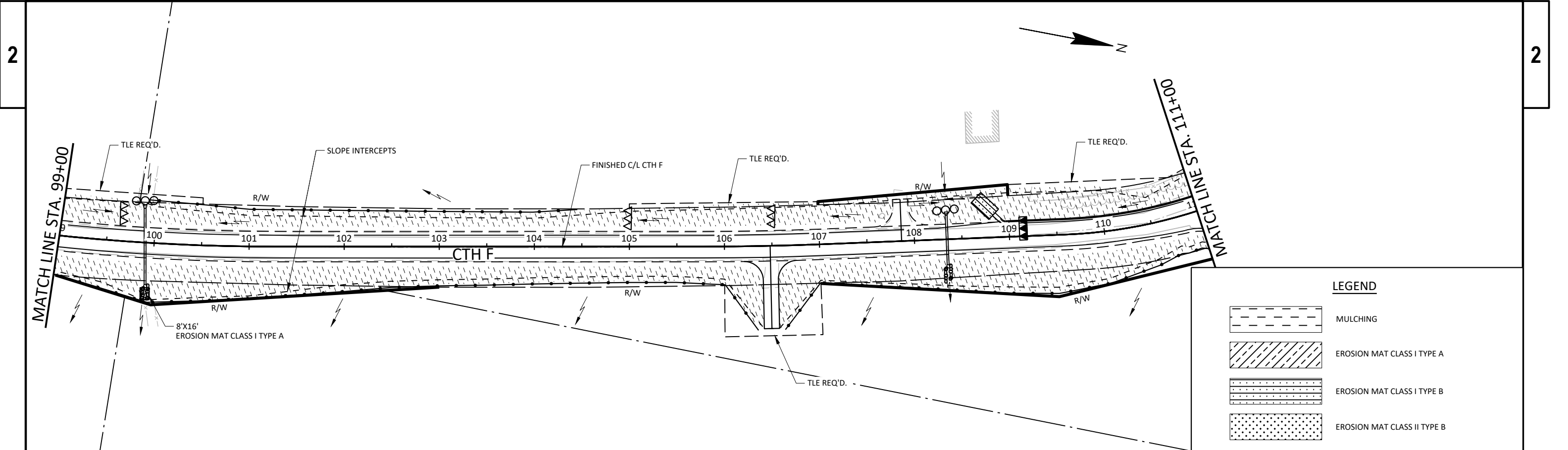
LEGEND

- MULCHING
- EROSION MAT CLASS I TYPE A
- EROSION MAT CLASS I TYPE B
- EROSION MAT CLASS II TYPE B
- SILT FENCE
- TEMPORARY DITCH CHECK
- TEMPORARY STONE DITCH CHECK
- CULVERT PIPE CHECK
- FLOW ARROW
- RIPRAP HEAVY OVER GEOTEXTILE TYPE HR
- EROSION MAT CLASS I TYPE B TREATMENT AT ASPHALTIC FLUMES, W=6' L=12' SEE CONSTRUCTION DETAILS
- WETLANDS



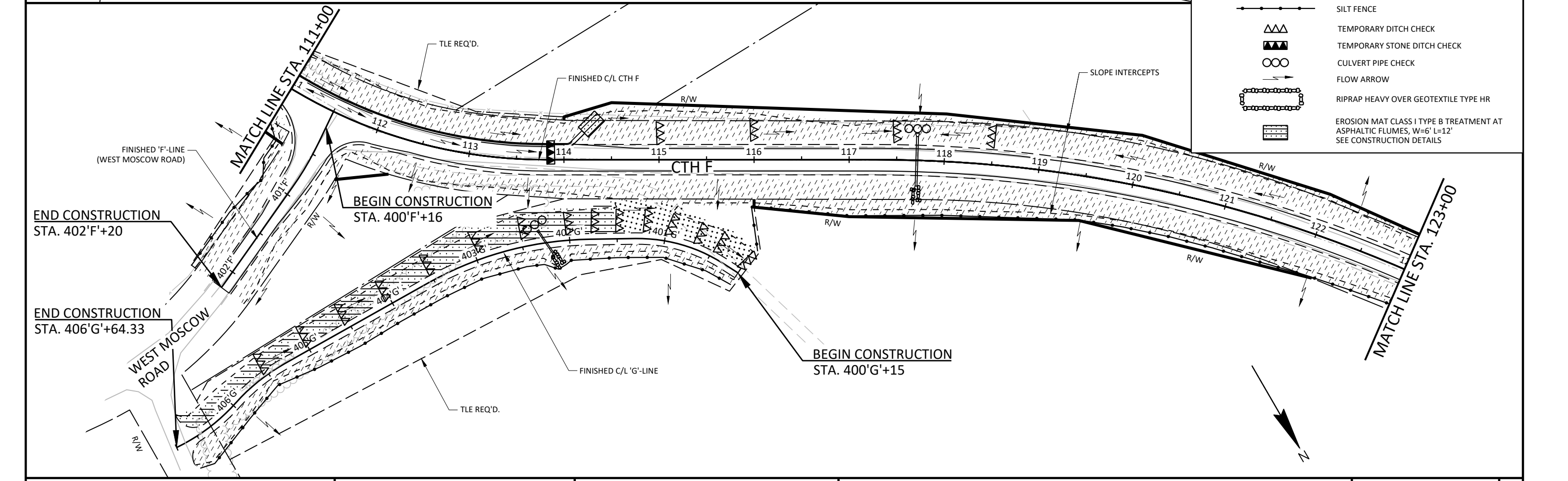
LEGEND

-  MULCHING
-  EROSION MAT CLASS I TYPE A
-  EROSION MAT CLASS I TYPE B
-  EROSION MAT CLASS II TYPE B
-  SILT FENCE
-  TEMPORARY DITCH CHECK
-  TEMPORARY STONE DITCH CHECK
-  CULVERT PIPE CHECK
-  FLOW ARROW
-  RIPRAP HEAVY OVER GEOTEXTILE TYPE HR
-  EROSION MAT CLASS I TYPE B TREATMENT AT ASPHALTIC FLUMES, W=6' L=12' SEE CONSTRUCTION DETAILS

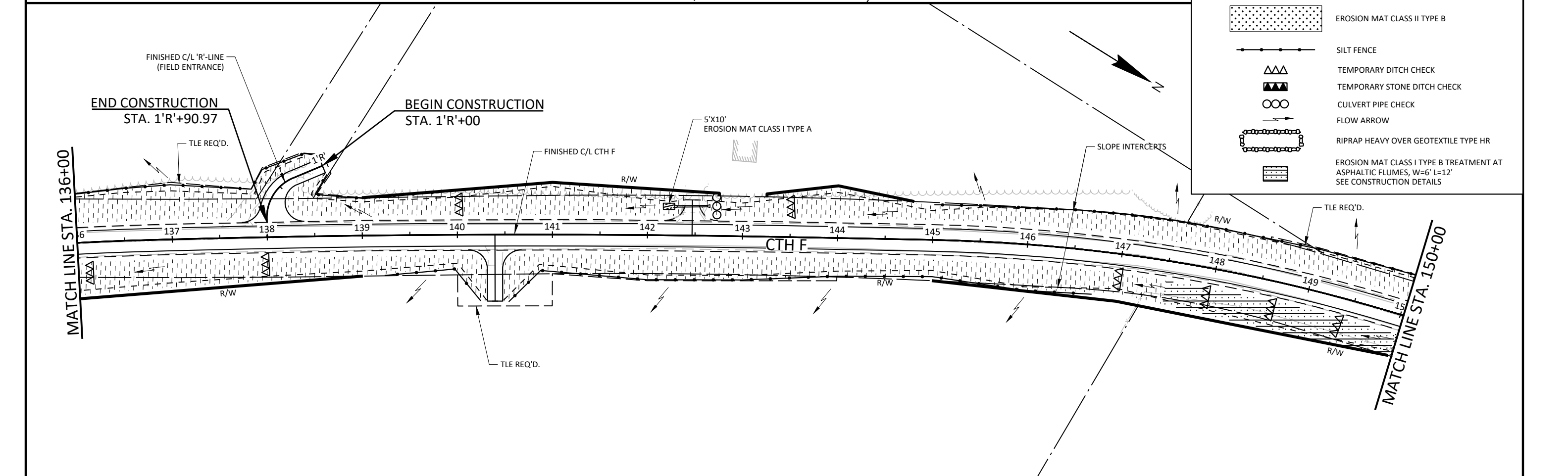
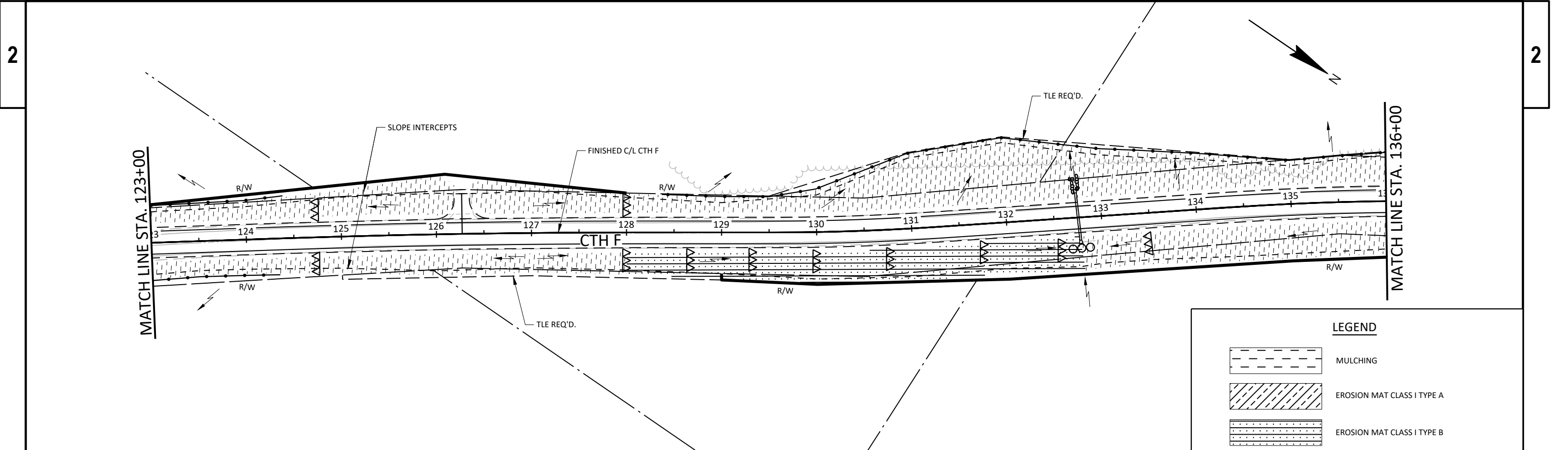


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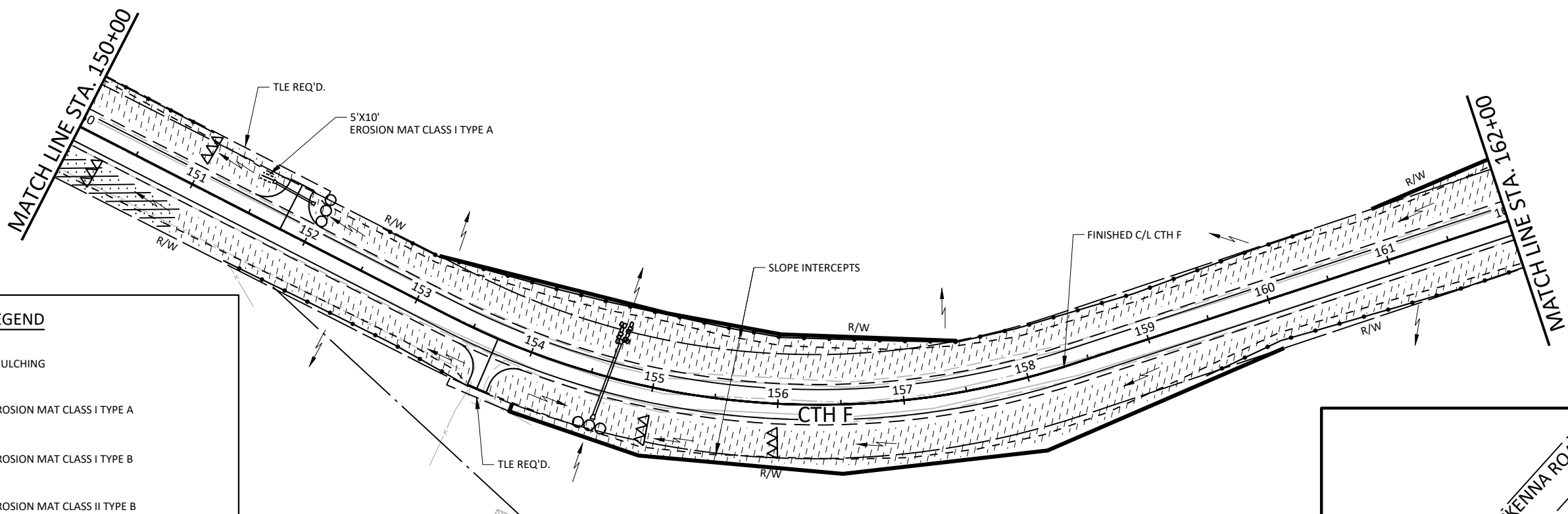
	MULCHING
	EROSION MAT CLASS I TYPE A
	EROSION MAT CLASS I TYPE B
	EROSION MAT CLASS II TYPE B
	SILT FENCE
	TEMPORARY DITCH CHECK
	TEMPORARY STONE DITCH CHECK
	CULVERT PIPE CHECK
	FLOW ARROW
	RIPRAP HEAVY OVER GEOTEXTILE TYPE HR
	EROSION MAT CLASS I TYPE B TREATMENT AT ASPHALTIC FLUMES, W=6' L=12' SEE CONSTRUCTION DETAILS



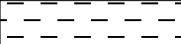
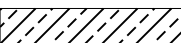
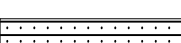
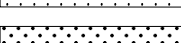





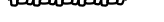

PROJECT NO: 5034-00-72 HWY: CTH F COUNTY: IOWA EROSION CONTROL PLAN SHEET E

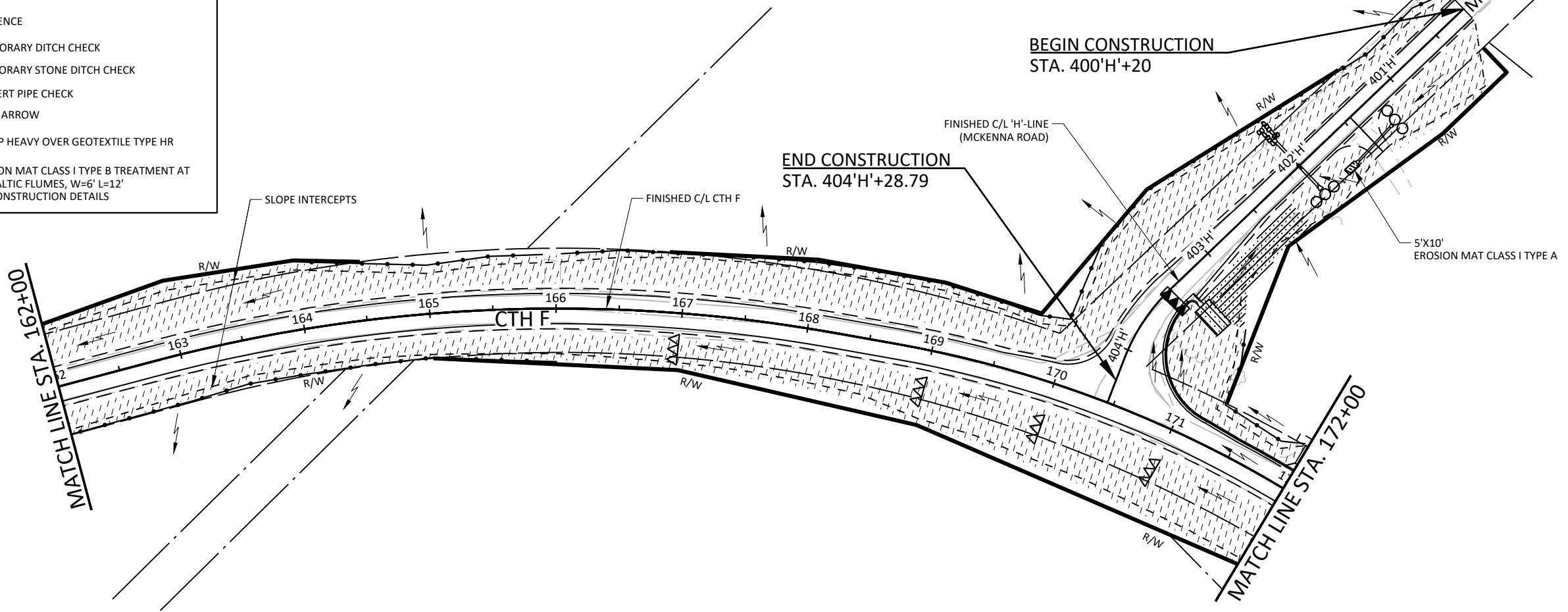


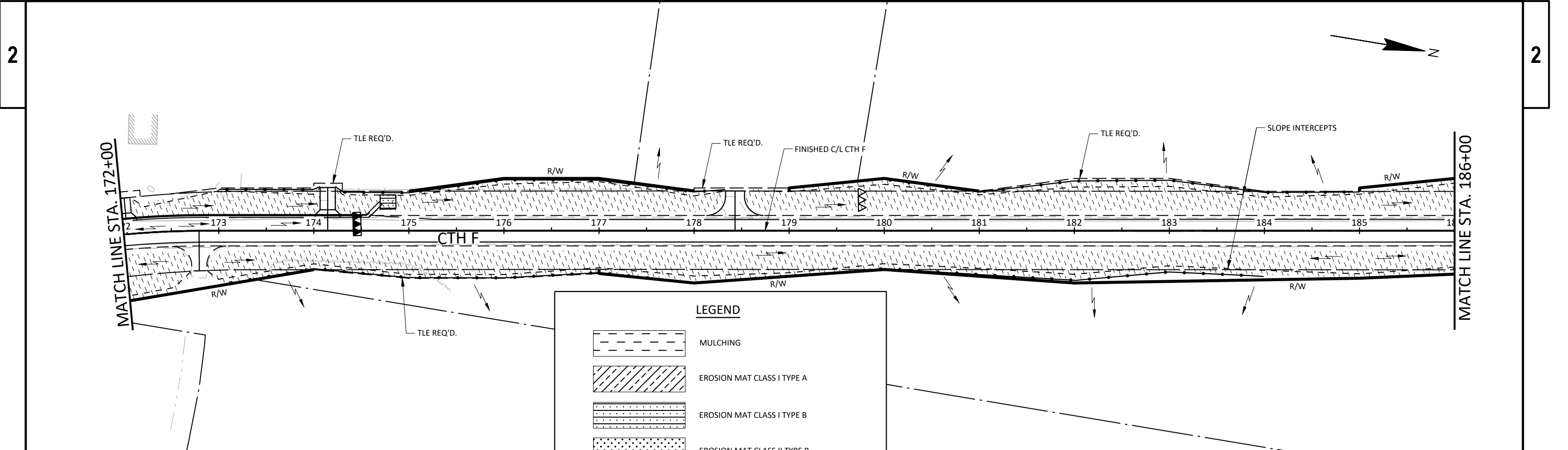
LEGEND	
	MULCHING
	EROSION MAT CLASS I TYPE A
	EROSION MAT CLASS I TYPE B
	EROSION MAT CLASS II TYPE B
	SILT FENCE
	TEMPORARY DITCH CHECK
	TEMPORARY STONE DITCH CHECK
	CULVERT PIPE CHECK
	FLOW ARROW
	RIPRAP HEAVY OVER GEOTEXTILE TYPE HR
	EROSION MAT CLASS I TYPE B TREATMENT AT ASPHALTIC FLUMES, W=6' L=12' SEE CONSTRUCTION DETAILS



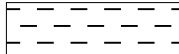

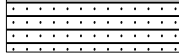

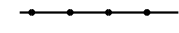



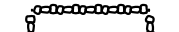
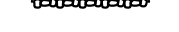

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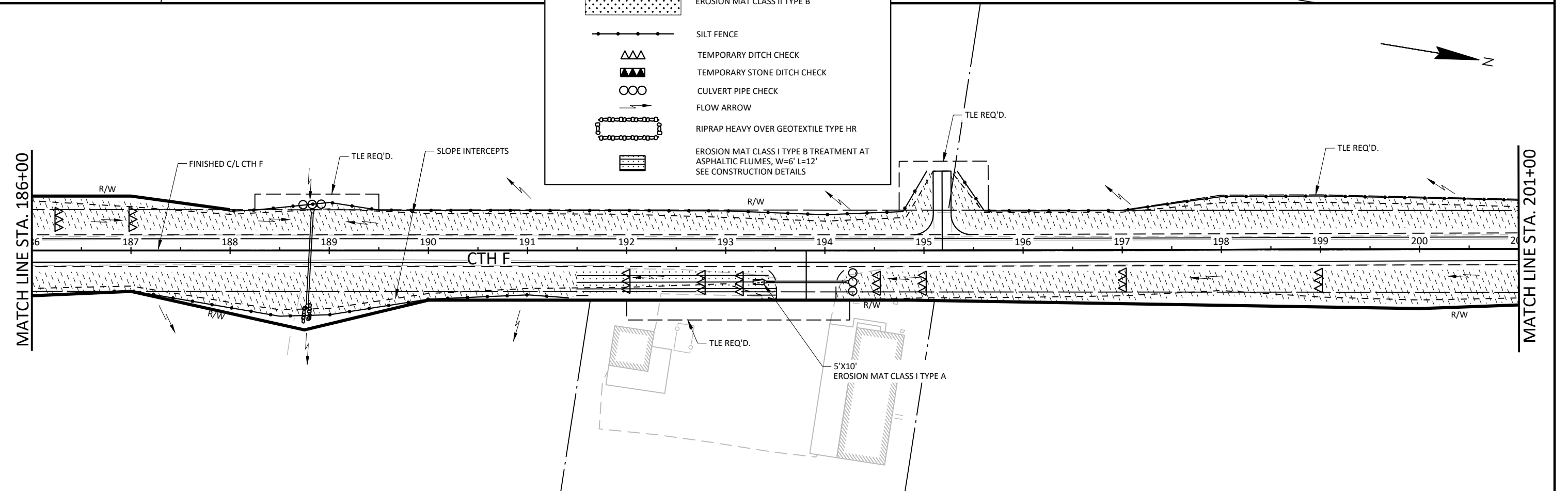
-  MULCHING
-  EROSION MAT CLASS I TYPE A
-  EROSION MAT CLASS I TYPE B
-  EROSION MAT CLASS II TYPE B
-  SILT FENCE
-  TEMPORARY DITCH CHECK
-  TEMPORARY STONE DITCH CHECK
-  CULVERT PIPE CHECK
-  FLOW ARROW
-  RIPRAP HEAVY OVER GEOTEXTILE TYPE HR
-  EROSION MAT CLASS I TYPE B TREATMENT AT ASPHALTIC FLUMES, W=6' L=12' SEE CONSTRUCTION DETAILS

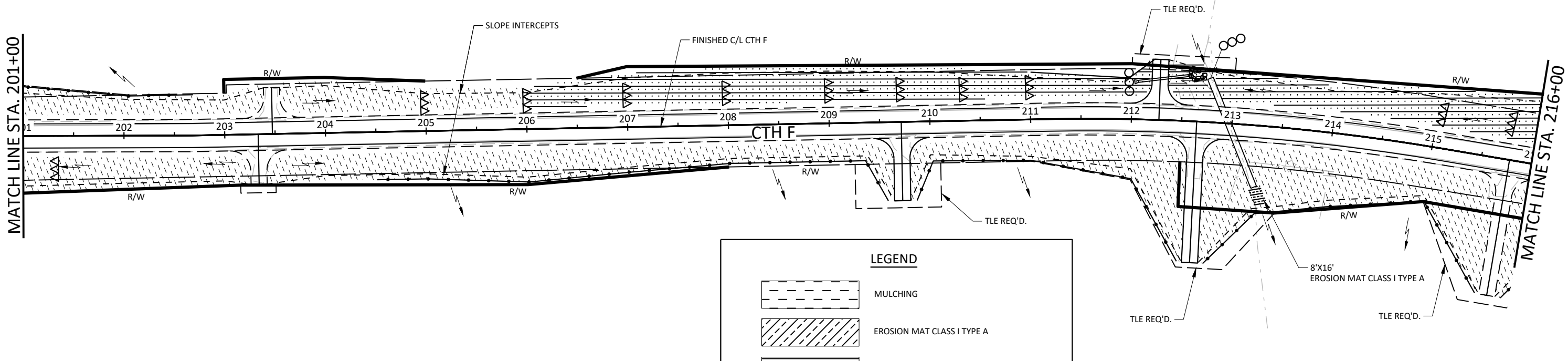




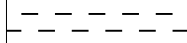
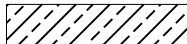
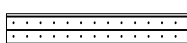
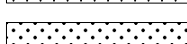
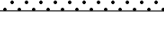




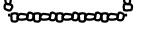

LEGEND

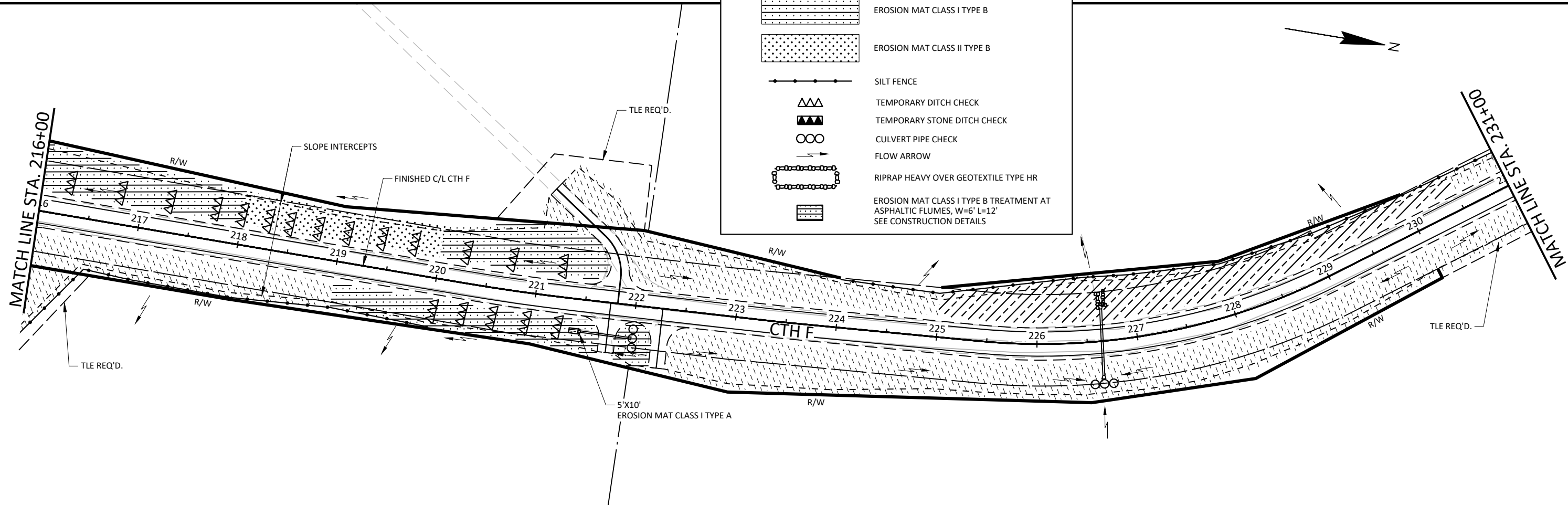
-  MULCHING
-  EROSION MAT CLASS I TYPE A
-  EROSION MAT CLASS I TYPE B
-  EROSION MAT CLASS II TYPE B
-  SILT FENCE
-  TEMPORARY DITCH CHECK
-  TEMPORARY STONE DITCH CHECK
-  CULVERT PIPE CHECK
-  FLOW ARROW
-  RIPRAP HEAVY OVER GEOTEXTILE TYPE HR
-  EROSION MAT CLASS I TYPE B TREATMENT AT ASPHALTIC FLUMES, W=6' L=12' SEE CONSTRUCTION DETAILS



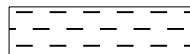
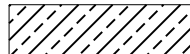
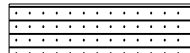
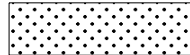
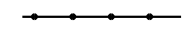




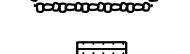



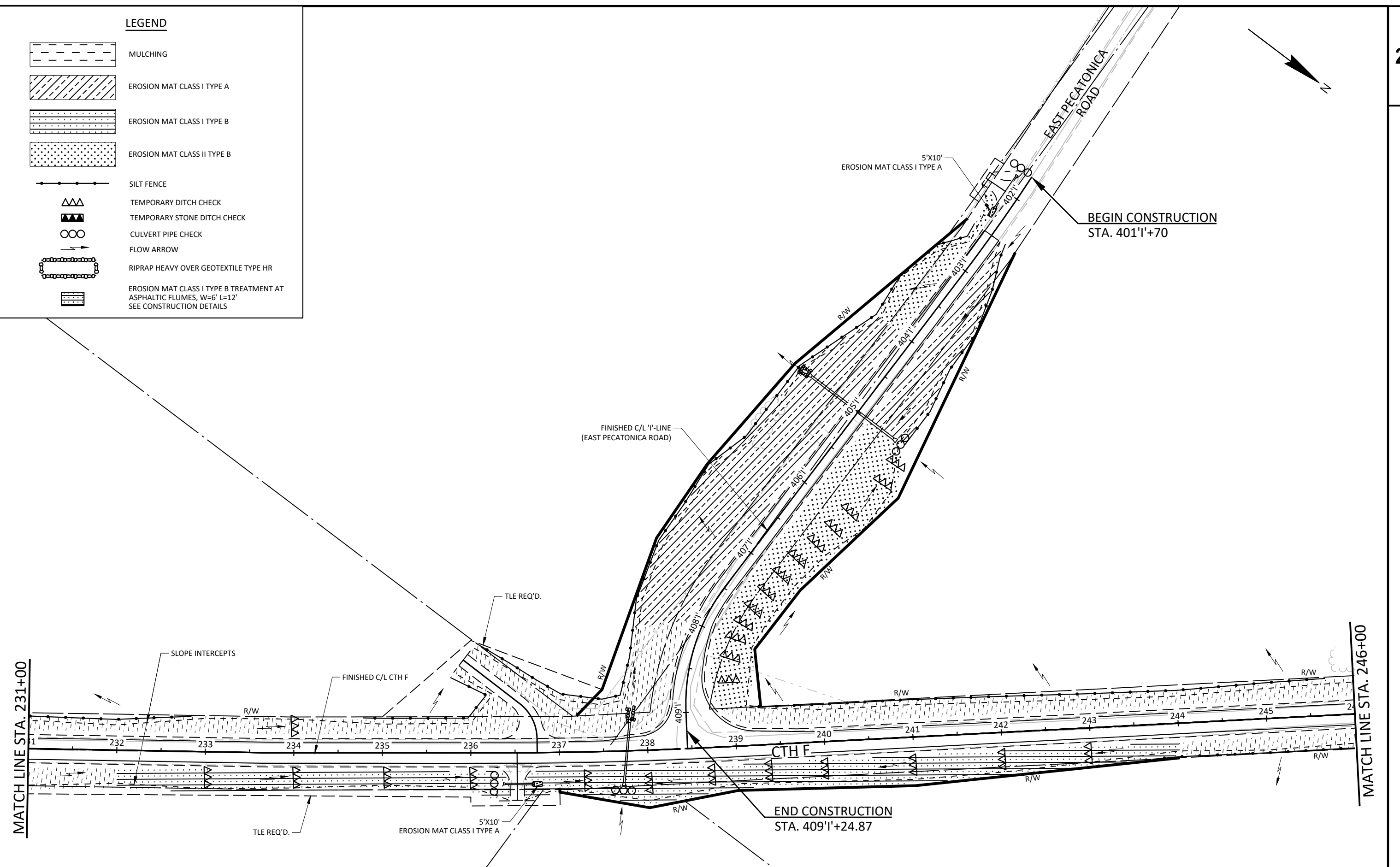
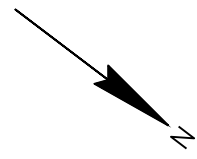
LEGEND

	MULCHING
	EROSION MAT CLASS I TYPE A
	EROSION MAT CLASS I TYPE B
	EROSION MAT CLASS II TYPE B
	SILT FENCE
	TEMPORARY DITCH CHECK
	TEMPORARY STONE DITCH CHECK
	CULVERT PIPE CHECK
	FLOW ARROW
	RIPRAP HEAVY OVER GEOTEXTILE TYPE HR
	EROSION MAT CLASS I TYPE B TREATMENT AT ASPHALTIC FLUMES, W=6' L=12' SEE CONSTRUCTION DETAILS

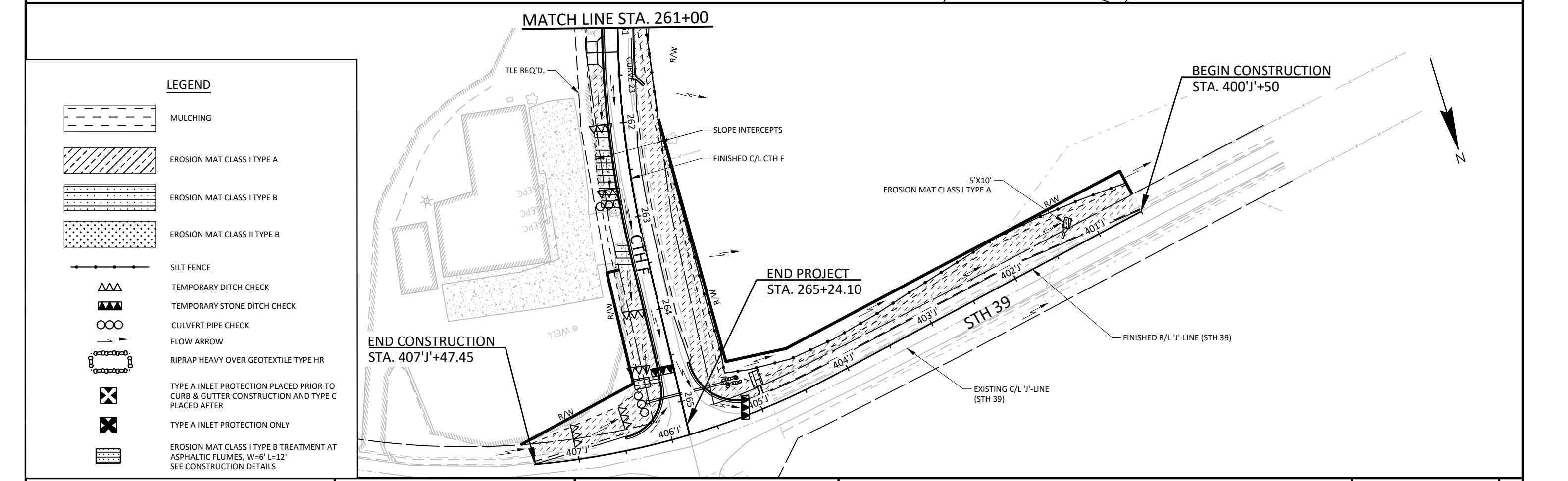
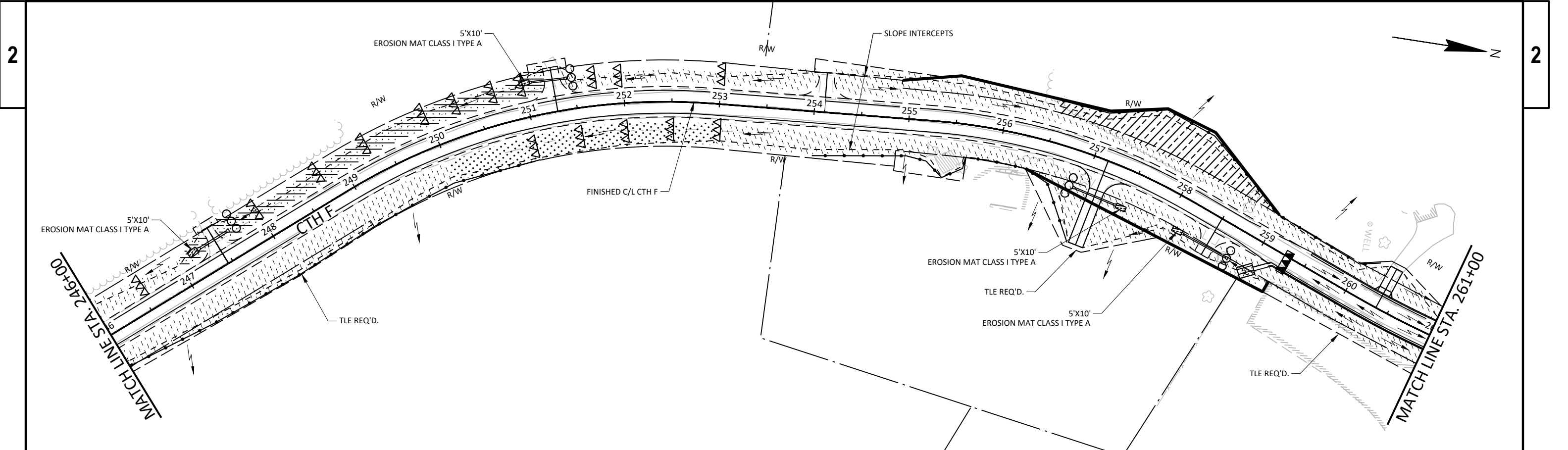


LEGEND

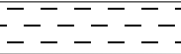
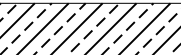
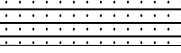



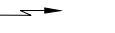
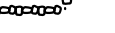
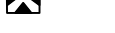


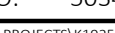

-  MULCHING
-  EROSION MAT CLASS I TYPE A
-  EROSION MAT CLASS I TYPE B
-  EROSION MAT CLASS II TYPE B
-  SILT FENCE
-  TEMPORARY DITCH CHECK
-  TEMPORARY STONE DITCH CHECK
-  CULVERT PIPE CHECK
-  FLOW ARROW
-  RIPRAP HEAVY OVER GEOTEXTILE TYPE HR
-  EROSION MAT CLASS I TYPE B TREATMENT AT ASPHALTIC FLUMES, W=6' L=12' SEE CONSTRUCTION DETAILS



PROJECT NO: 5034-00-72	HWY: CTH F	COUNTY: IOWA	EROSION CONTROL PLAN	SHEET	E
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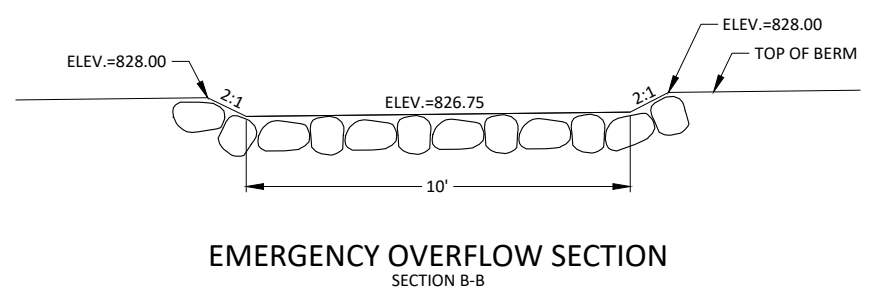
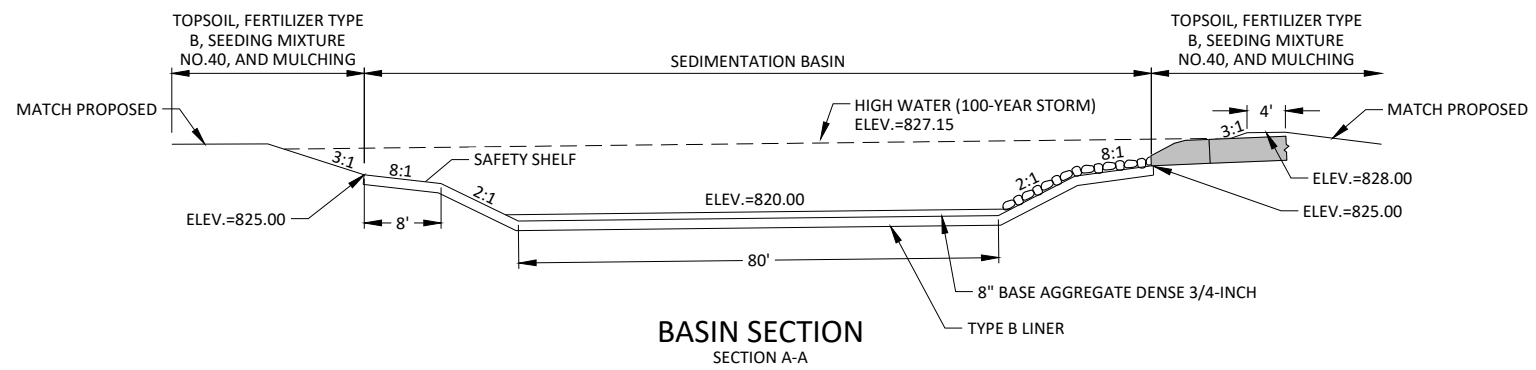
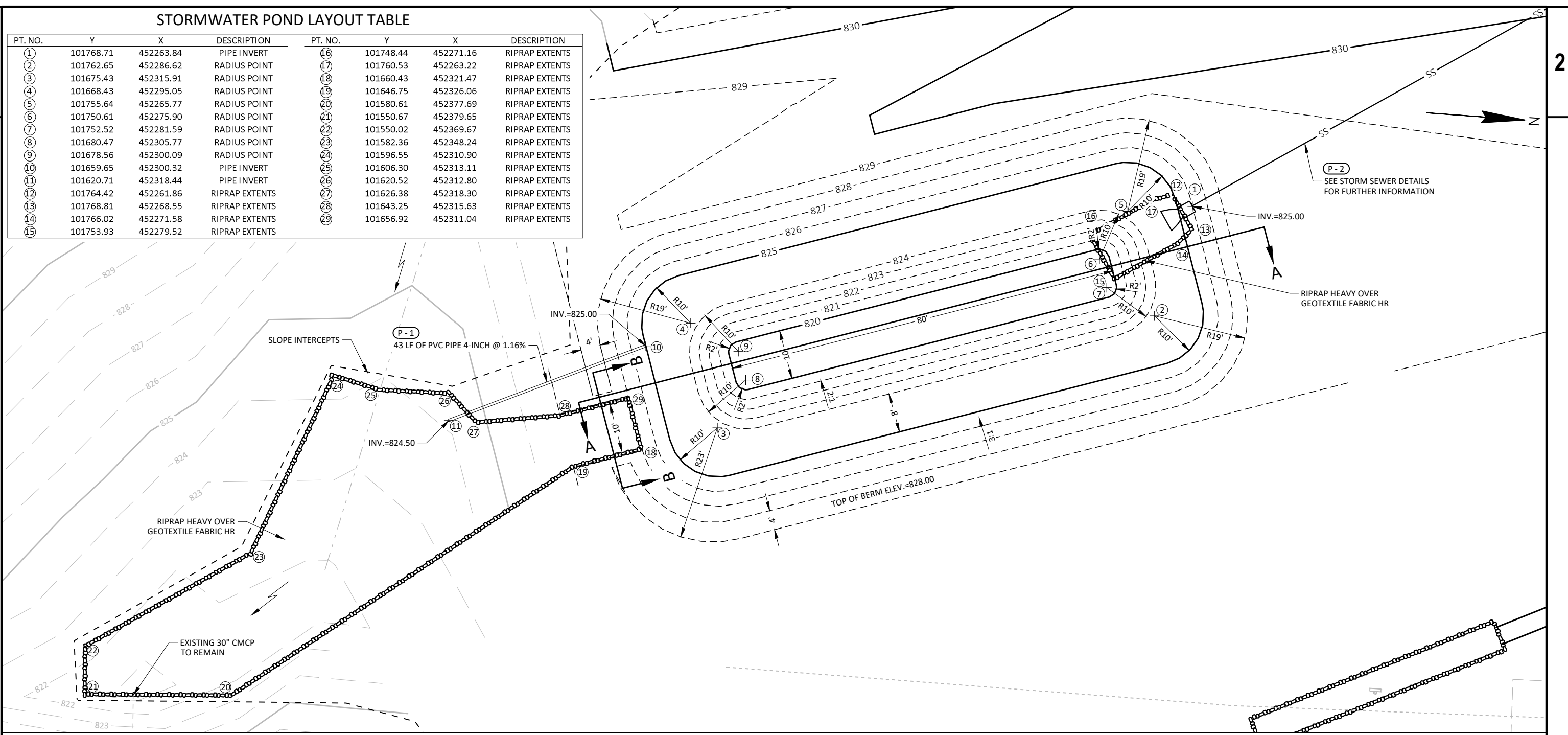


LEGEND

-  MULCHING
-  EROSION MAT CLASS I TYPE A
-  EROSION MAT CLASS I TYPE B
-  EROSION MAT CLASS II TYPE B
-  SILT FENCE
-  TEMPORARY DITCH CHECK
-  TEMPORARY STONE DITCH CHECK
-  CULVERT PIPE CHECK
-  FLOW ARROW
-  RIPRAP HEAVY OVER GEOTEXTILE TYPE HR
-  TYPE A INLET PROTECTION PLACED PRIOR TO CURB & GUTTER CONSTRUCTION AND TYPE C PLACED AFTER
-  TYPE A INLET PROTECTION ONLY
-  EROSION MAT CLASS I TYPE B TREATMENT AT ASPHALTIC FLUMES, W=6' L=12' SEE CONSTRUCTION DETAILS

STORMWATER POND LAYOUT TABLE

PT. NO.	Y	X	DESCRIPTION	PT. NO.	Y	X	DESCRIPTION
1	101768.71	452263.84	PIPE INVERT	16	101748.44	452271.16	RIPRAP EXTENTS
2	101762.65	452286.62	RADIUS POINT	17	101760.53	452263.22	RIPRAP EXTENTS
3	101675.43	452315.91	RADIUS POINT	18	101660.43	452321.47	RIPRAP EXTENTS
4	101668.43	452295.05	RADIUS POINT	19	101646.75	452326.06	RIPRAP EXTENTS
5	101755.64	452265.77	RADIUS POINT	20	101580.61	452377.69	RIPRAP EXTENTS
6	101750.61	452275.90	RADIUS POINT	21	101550.67	452379.65	RIPRAP EXTENTS
7	101752.52	452281.59	RADIUS POINT	22	101550.02	452369.67	RIPRAP EXTENTS
8	101680.47	452305.77	RADIUS POINT	23	101582.36	452348.24	RIPRAP EXTENTS
9	101678.56	452300.09	RADIUS POINT	24	101596.55	452310.90	RIPRAP EXTENTS
10	101659.65	452300.32	PIPE INVERT	25	101606.30	452313.11	RIPRAP EXTENTS
11	101620.71	452318.44	PIPE INVERT	26	101620.52	452312.80	RIPRAP EXTENTS
12	101764.42	452261.86	RIPRAP EXTENTS	27	101626.38	452318.30	RIPRAP EXTENTS
13	101768.81	452268.55	RIPRAP EXTENTS	28	101643.25	452315.63	RIPRAP EXTENTS
14	101766.02	452271.58	RIPRAP EXTENTS	29	101656.92	452311.04	RIPRAP EXTENTS
15	101753.93	452279.52	RIPRAP EXTENTS				

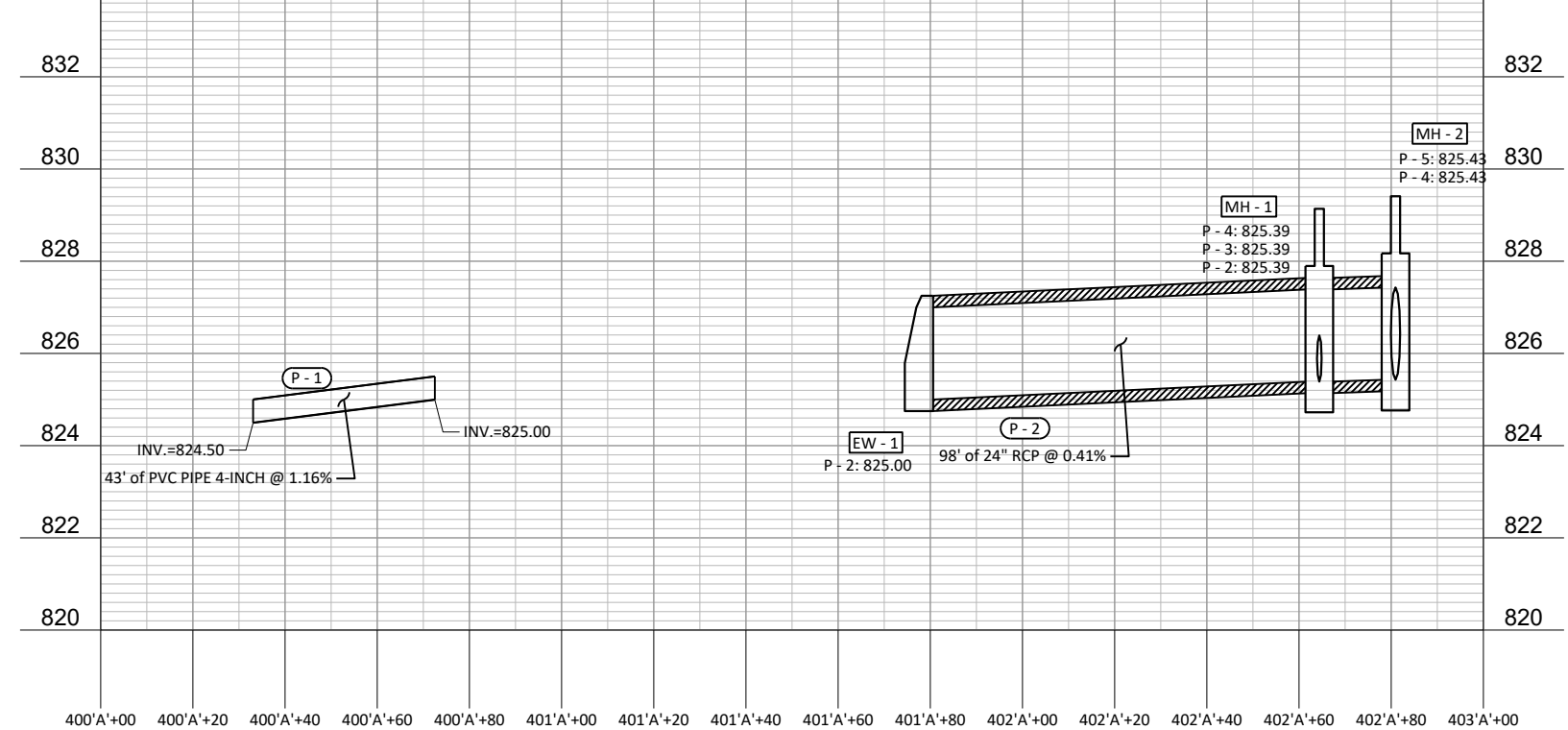
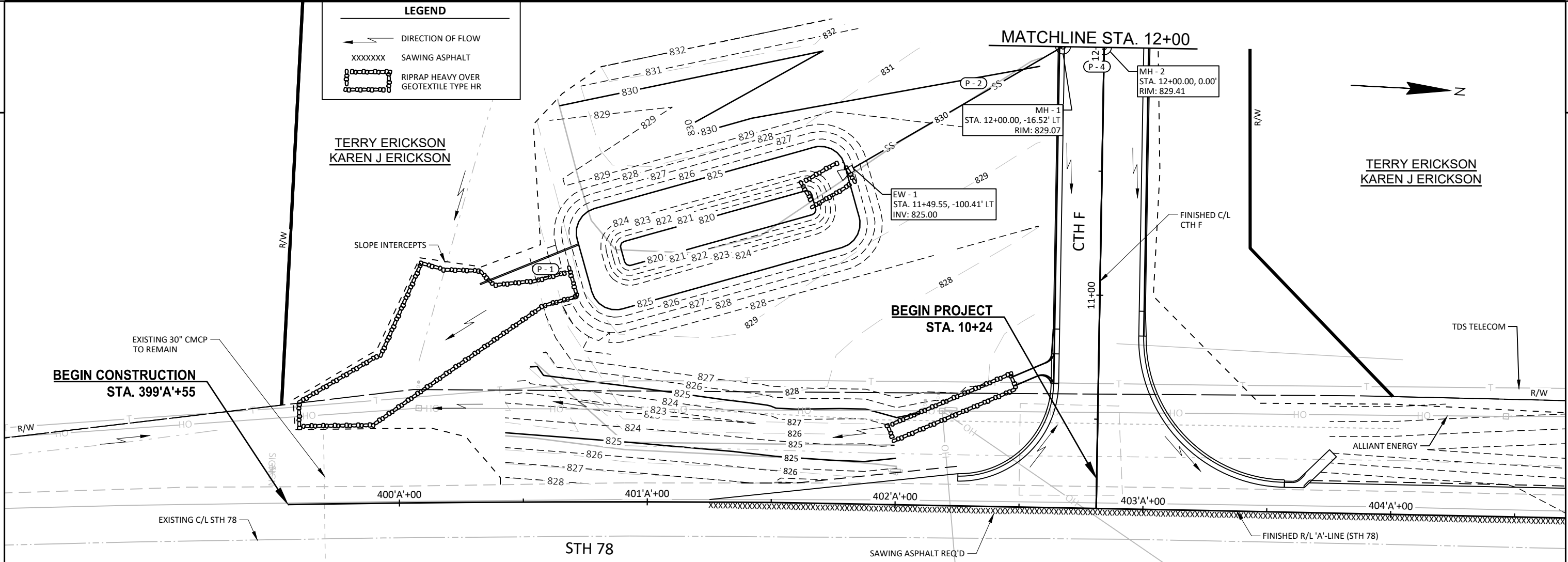


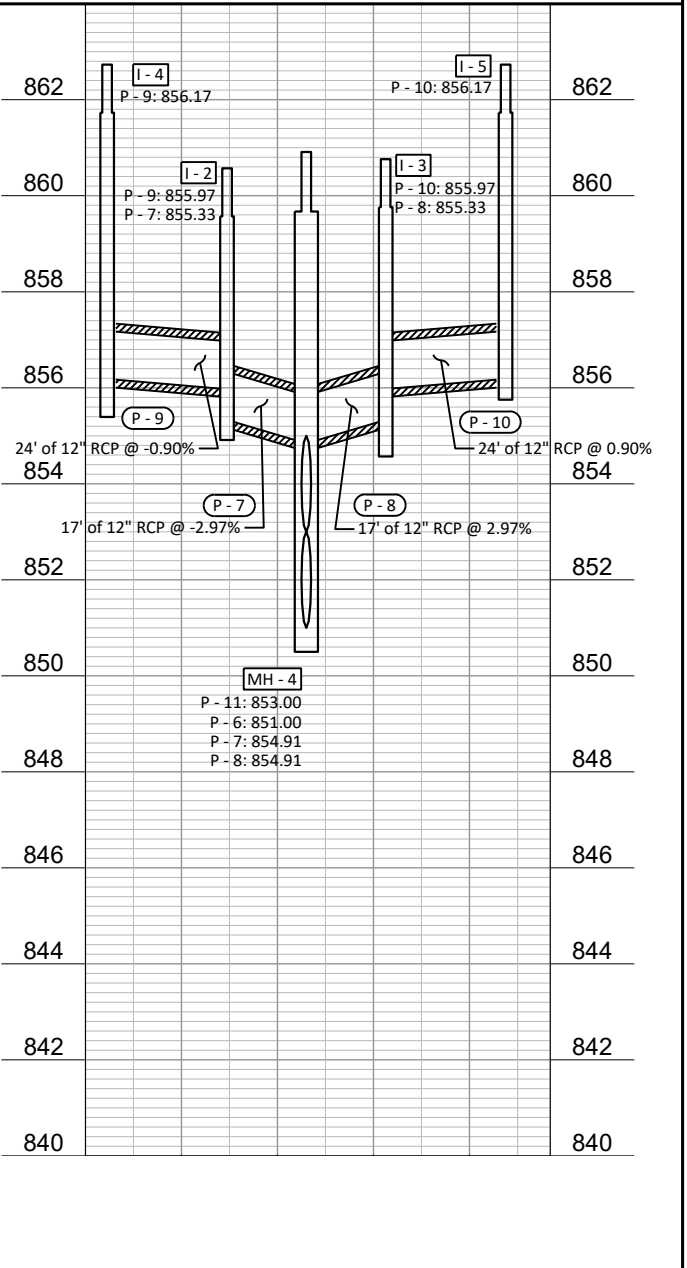
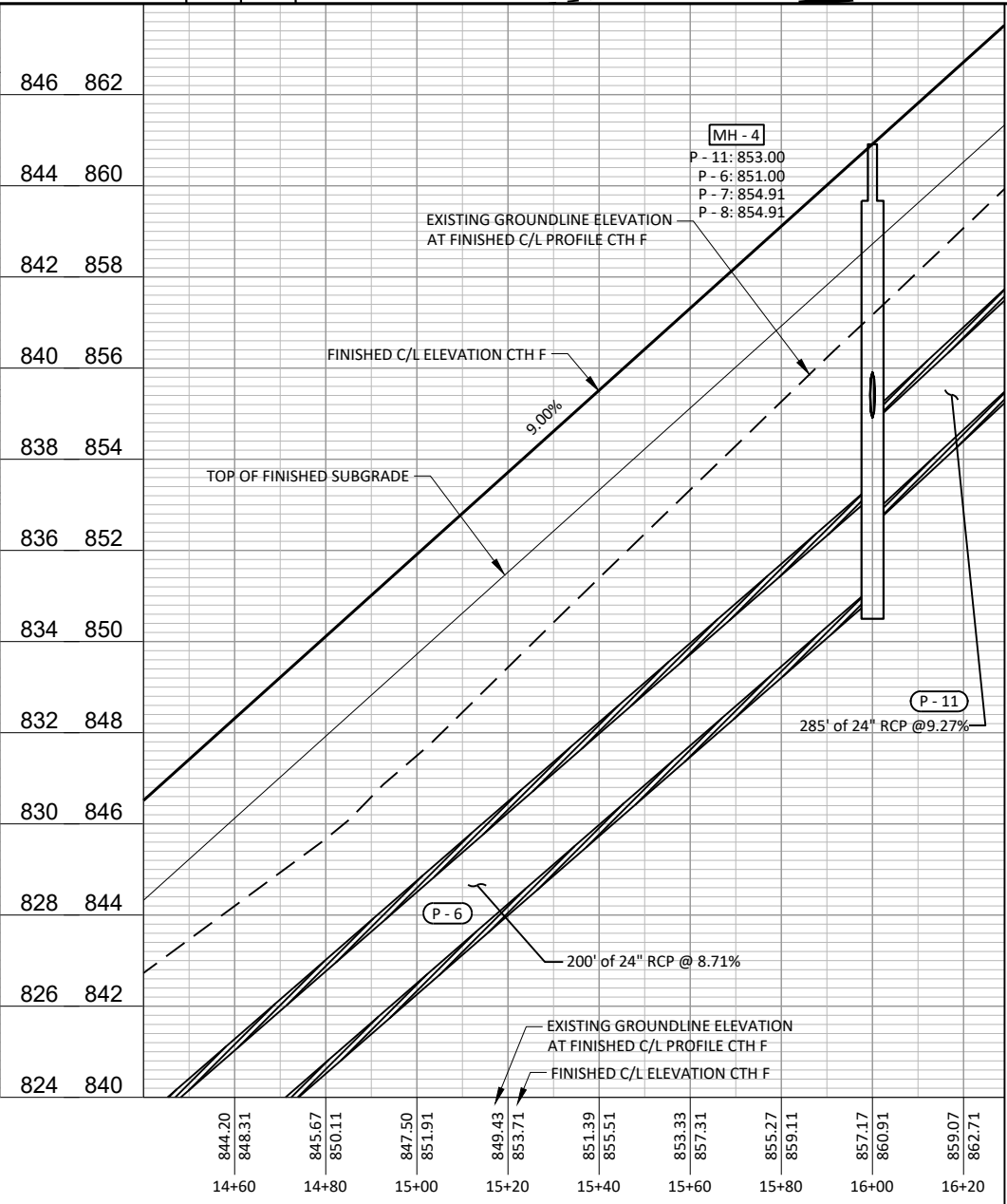
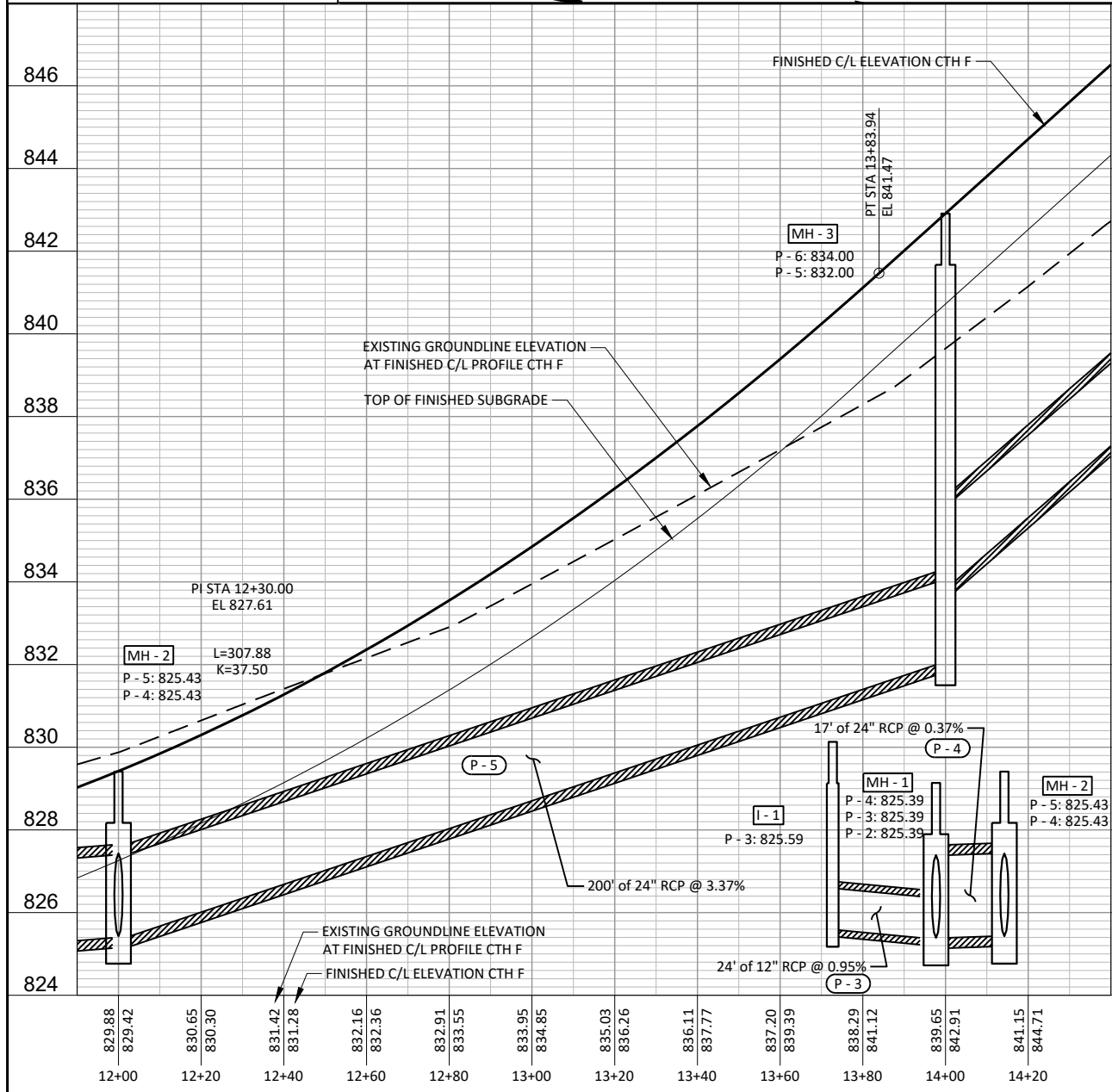
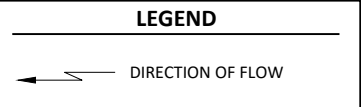
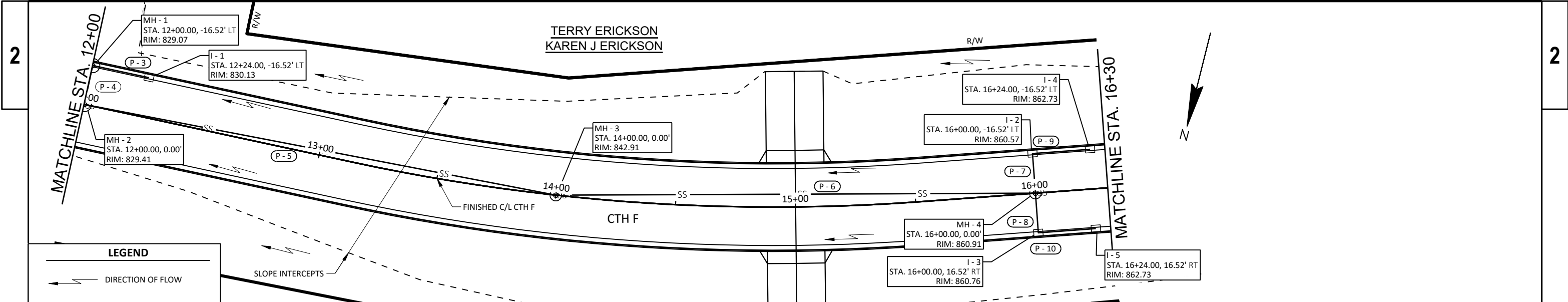
LEGEND

- DIRECTION OF FLOW
- XXXXXXX SAWING ASPHALT
- ▭ RIPRAP HEAVY OVER GEOTEXTILE TYPE HR

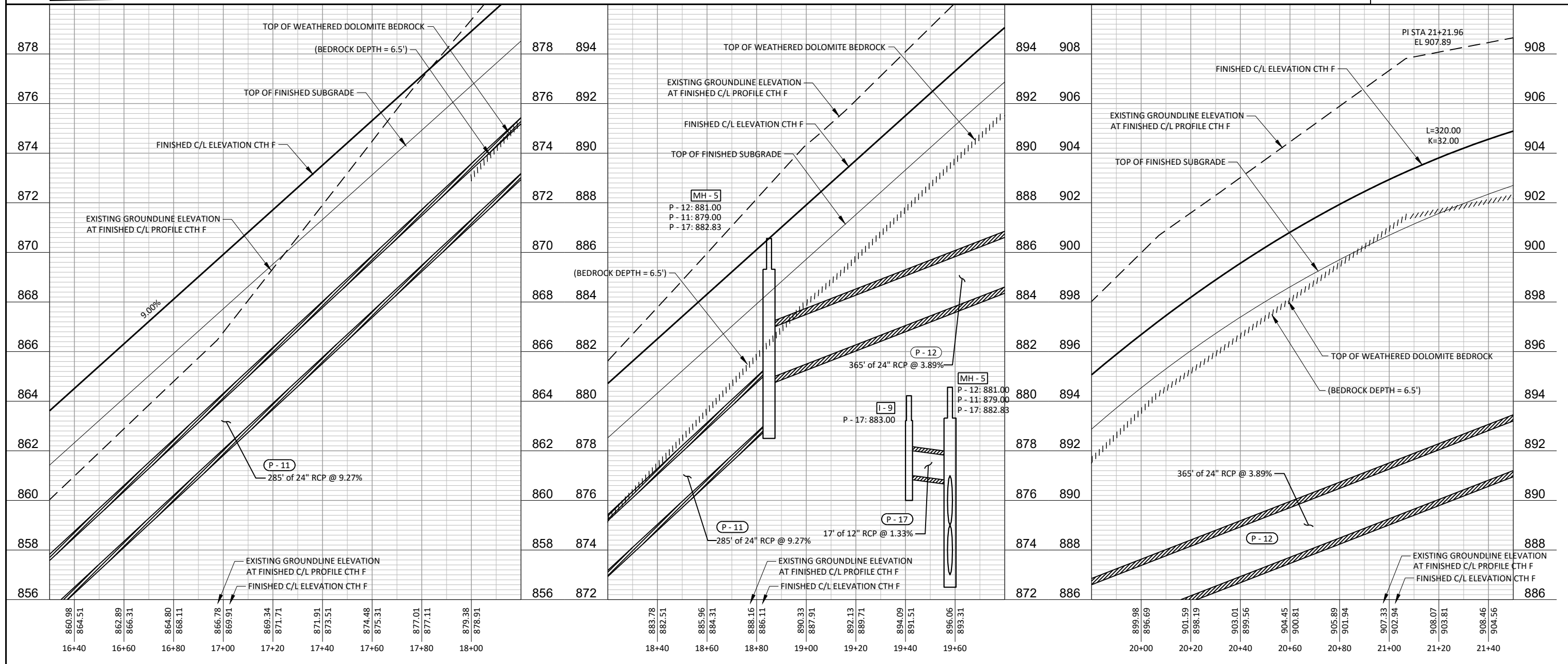
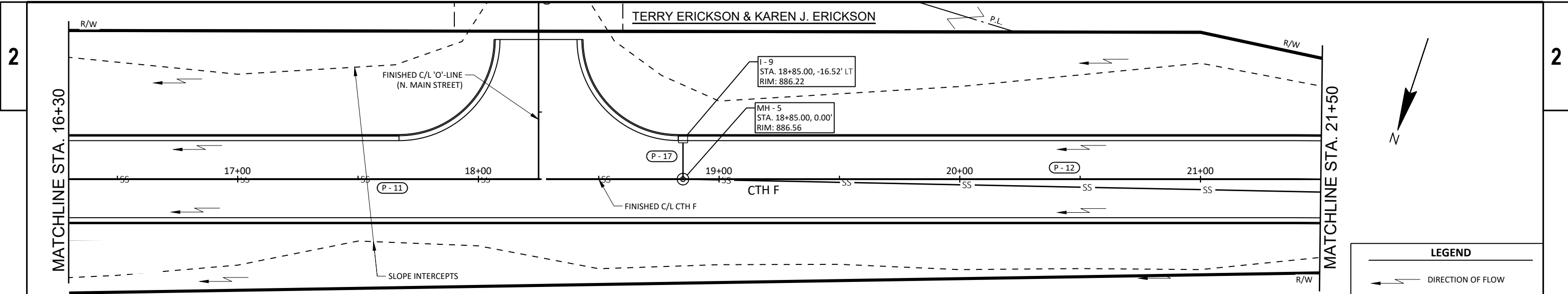
TERRY ERICKSON
KAREN J ERICKSON

TERRY ERICKSON
KAREN J ERICKSON





PROJECT NO: 5034-00-72	HWY: CTH F	COUNTY: IOWA	STORM SEWER DETAILS	SHEET	E
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PROJECT NO: 5034-00-72

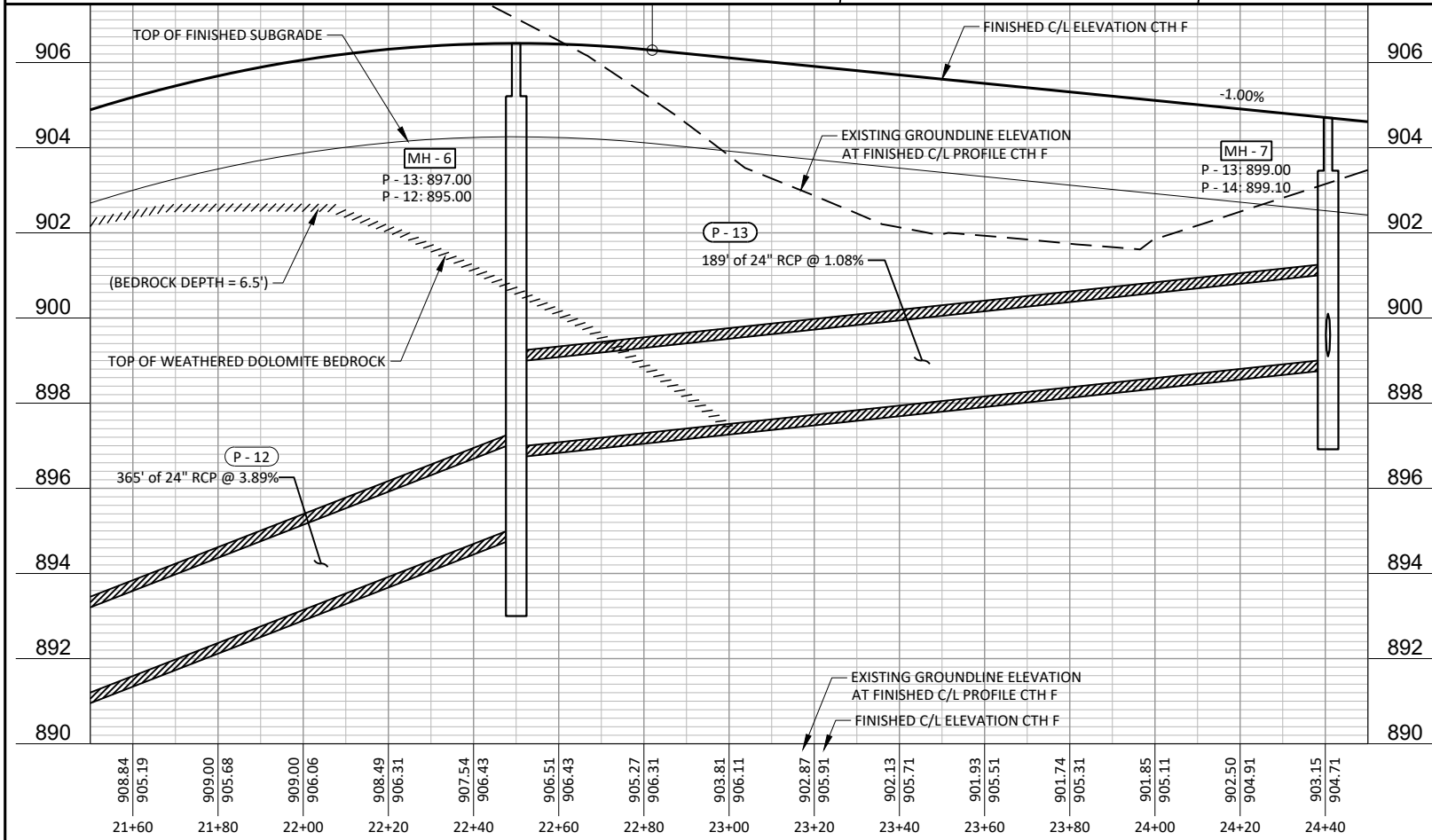
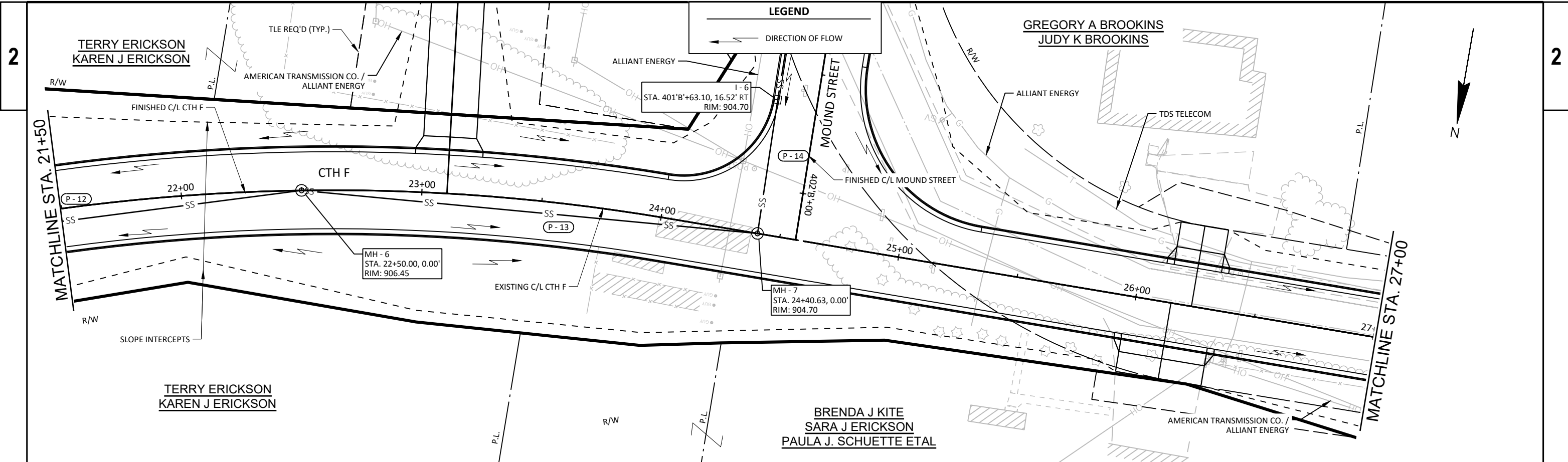
HWY: CTH F

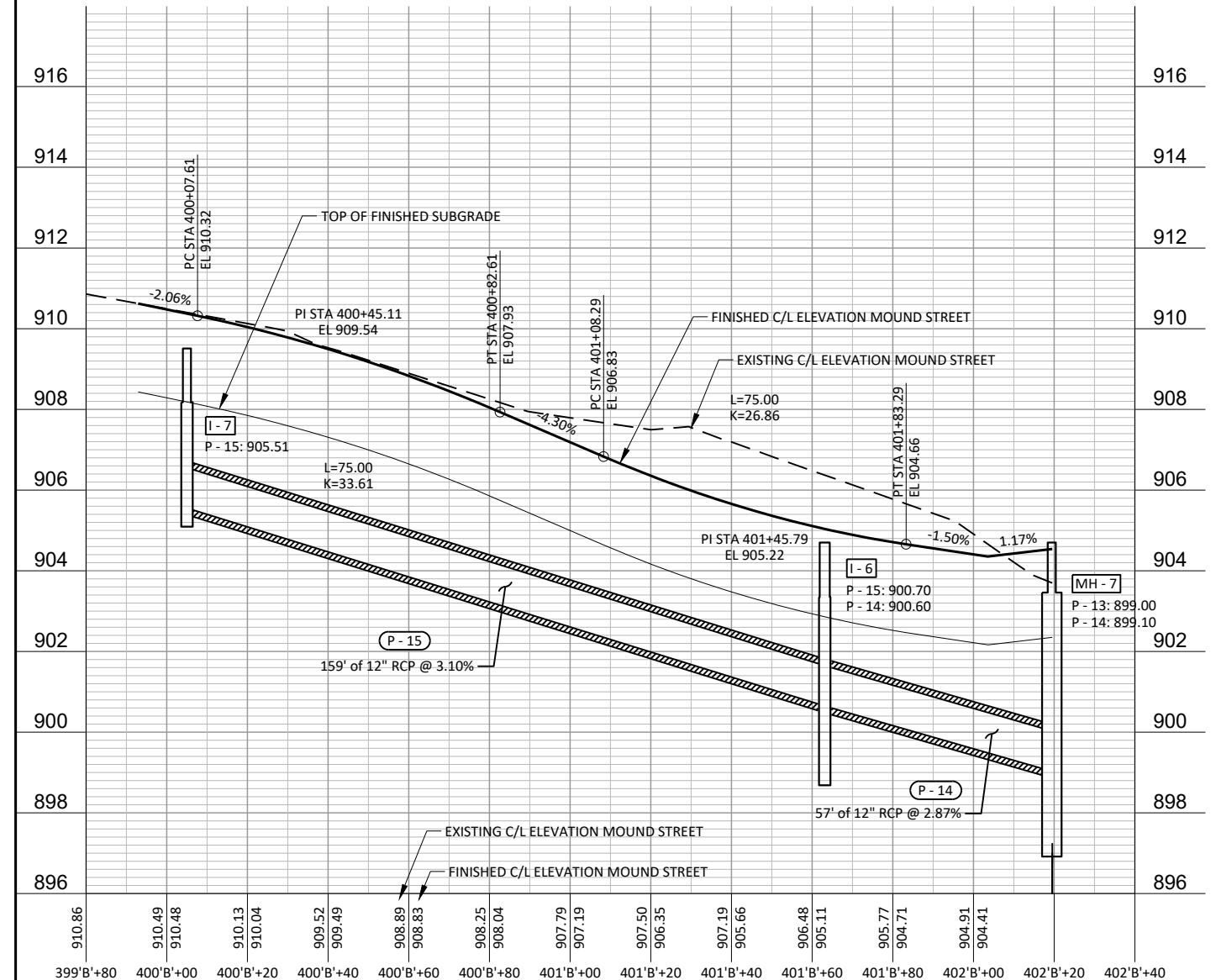
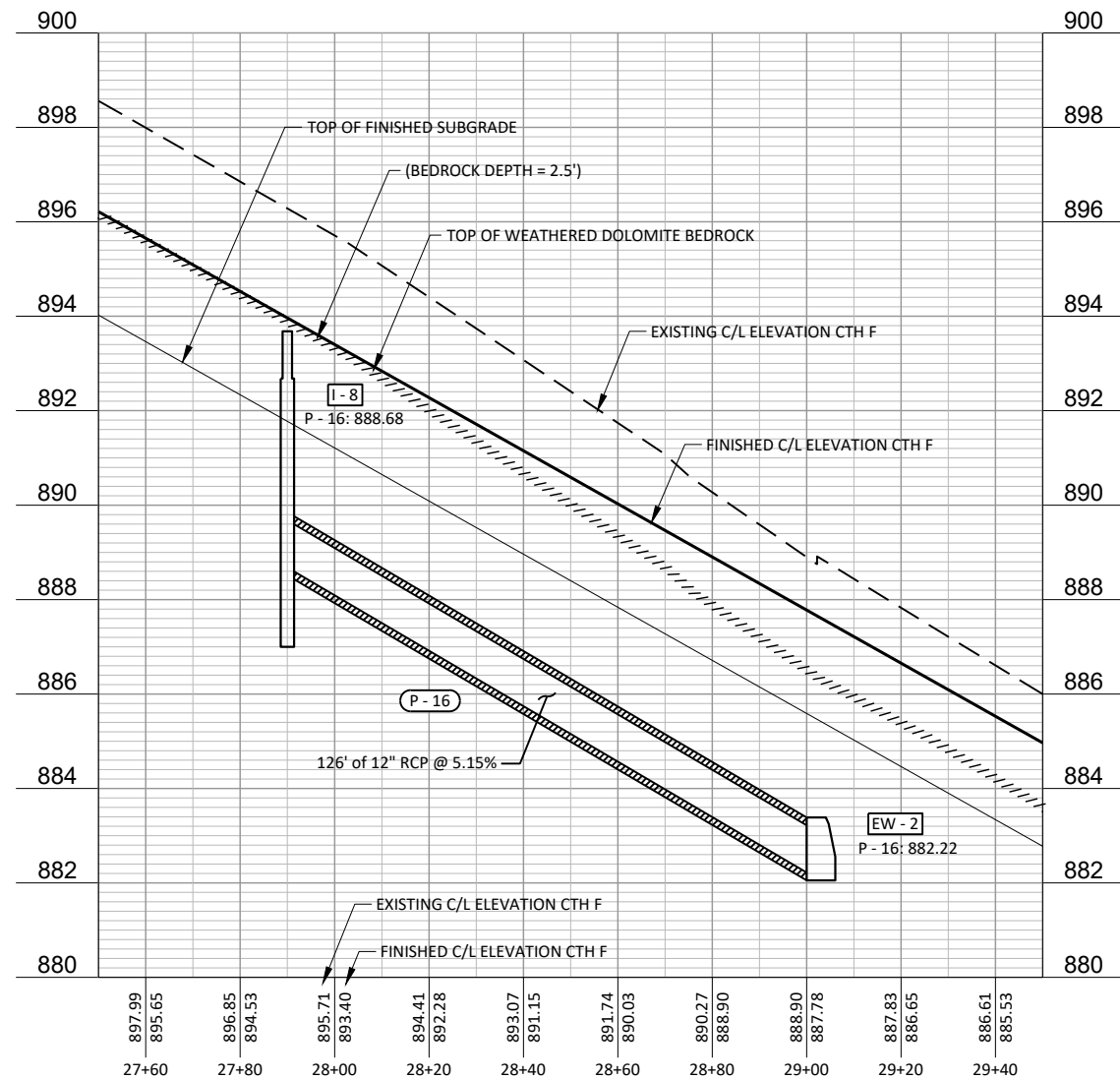
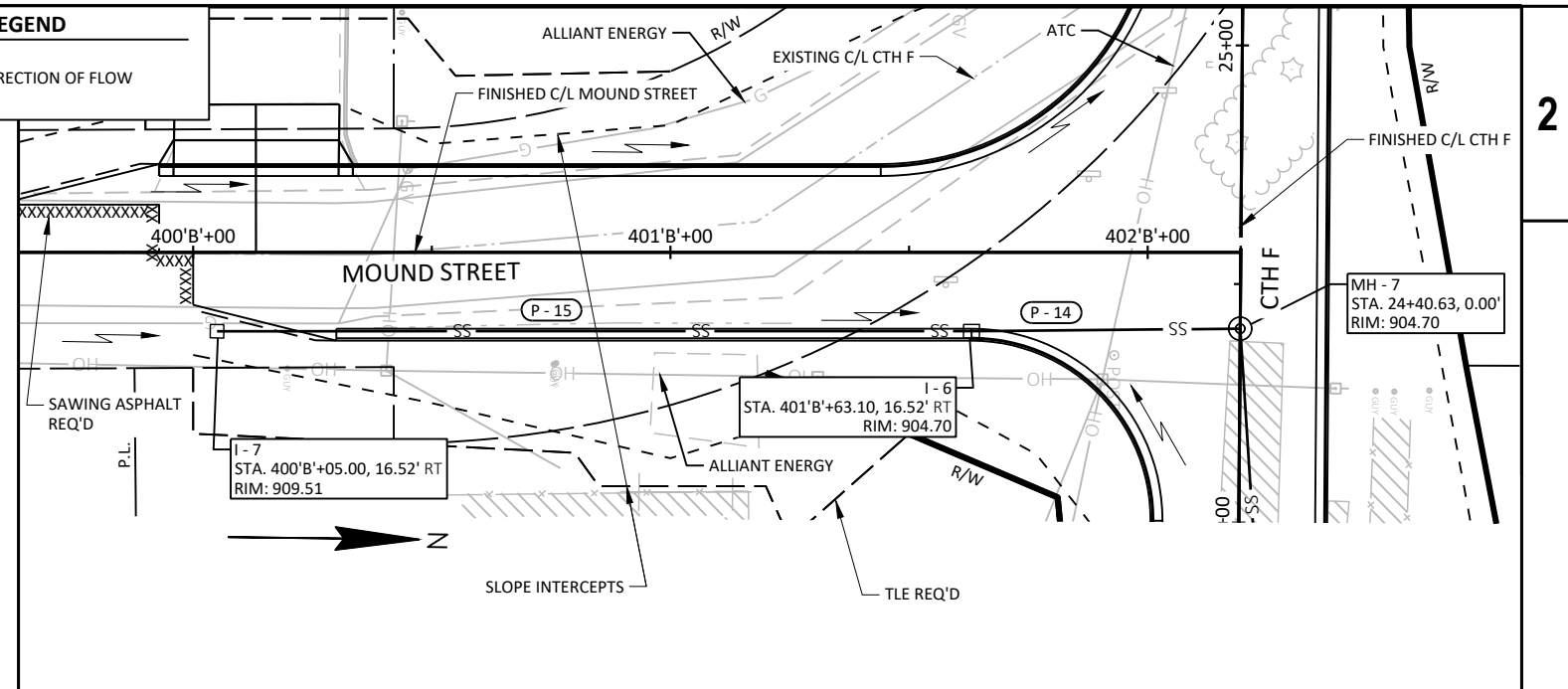
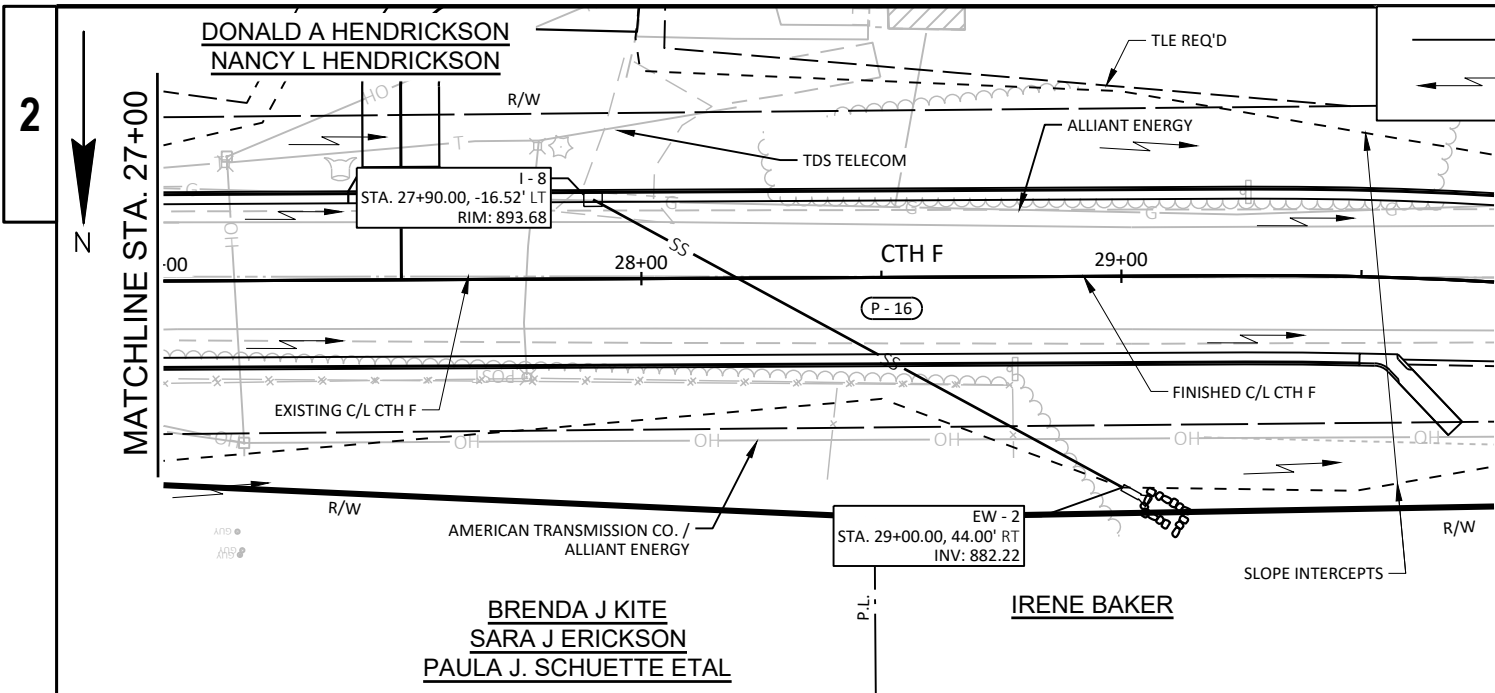
COUNTY: IOWA

STORM SEWER DETAILS

SHEET

E





PROJECT NO: 5034-00-72

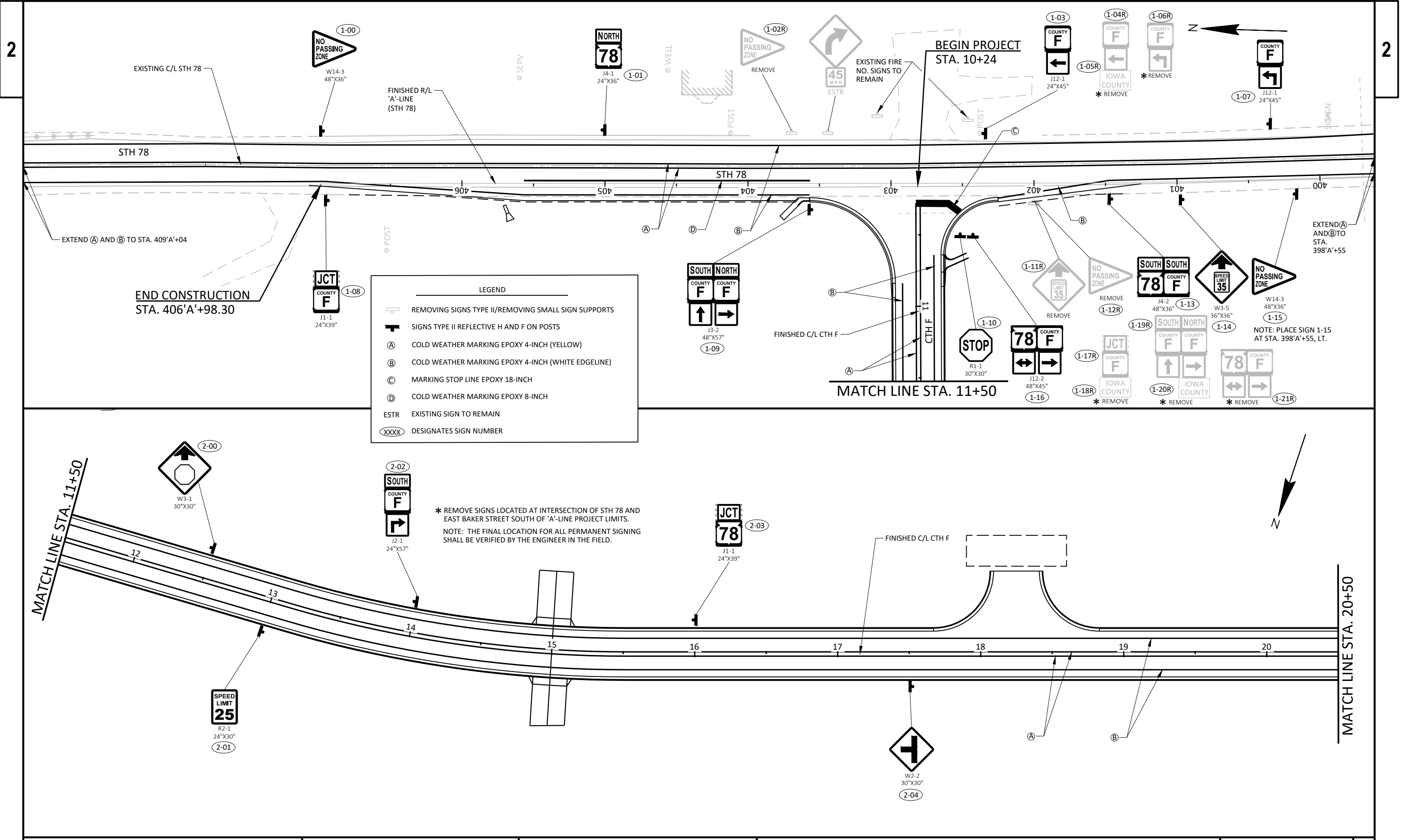
HWY: CTH F

COUNTY: IOWA







STORM SEWER DETAILS

SHEET

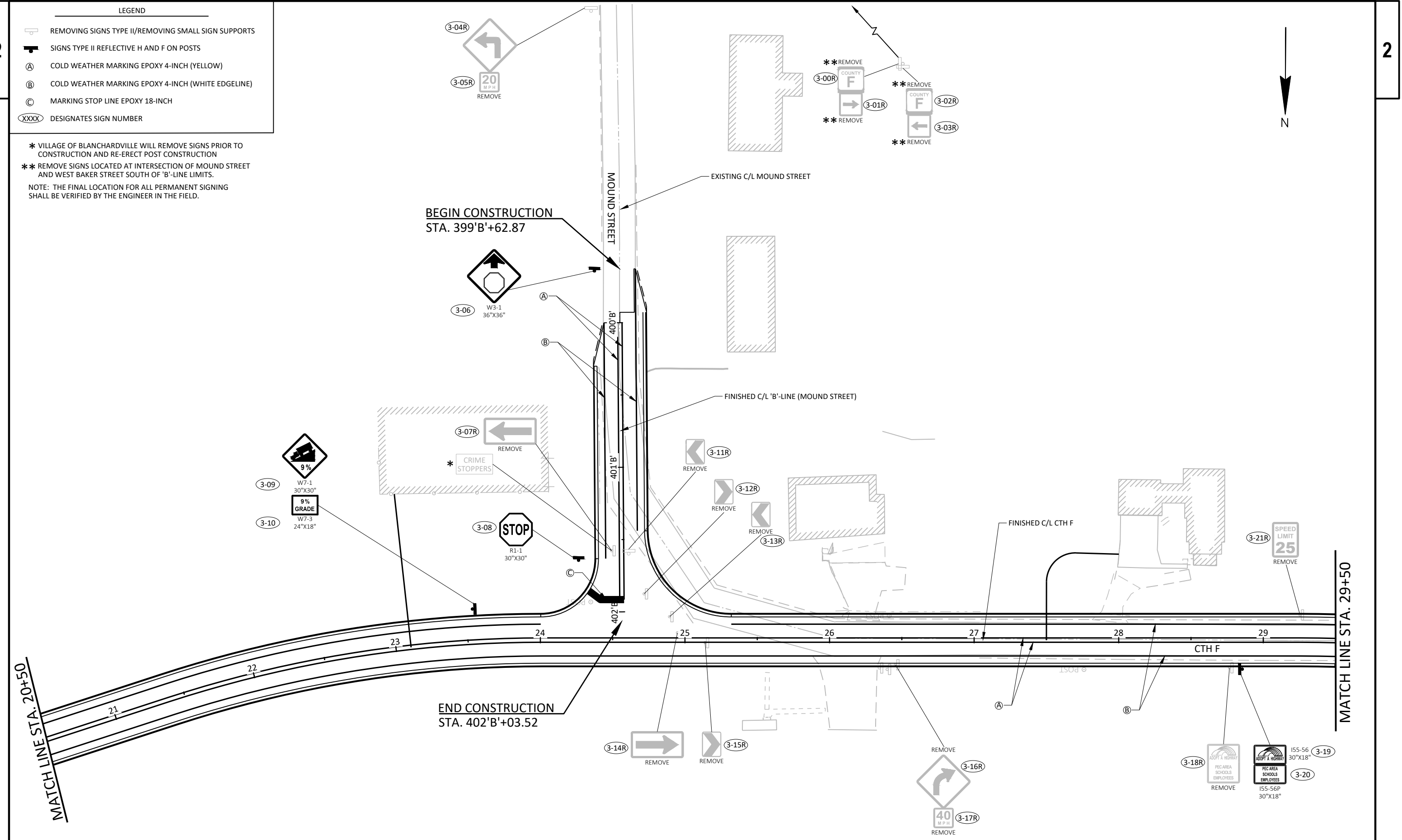
E








LEGEND

-  REMOVING SIGNS TYPE II/REMOVING SMALL SIGN SUPPORTS
-  SIGNS TYPE II REFLECTIVE H AND F ON POSTS
-  COLD WEATHER MARKING EPOXY 4-INCH (YELLOW)
-  COLD WEATHER MARKING EPOXY 4-INCH (WHITE EDGELINE)
-  MARKING STOP LINE EPOXY 18-INCH
-  DESIGNATES SIGN NUMBER

* VILLAGE OF BLANCHARDVILLE WILL REMOVE SIGNS PRIOR TO CONSTRUCTION AND RE-ERECT POST CONSTRUCTION
 ** REMOVE SIGNS LOCATED AT INTERSECTION OF MOUND STREET AND WEST BAKER STREET SOUTH OF 'B'-LINE LIMITS.
 NOTE: THE FINAL LOCATION FOR ALL PERMANENT SIGNING SHALL BE VERIFIED BY THE ENGINEER IN THE FIELD.



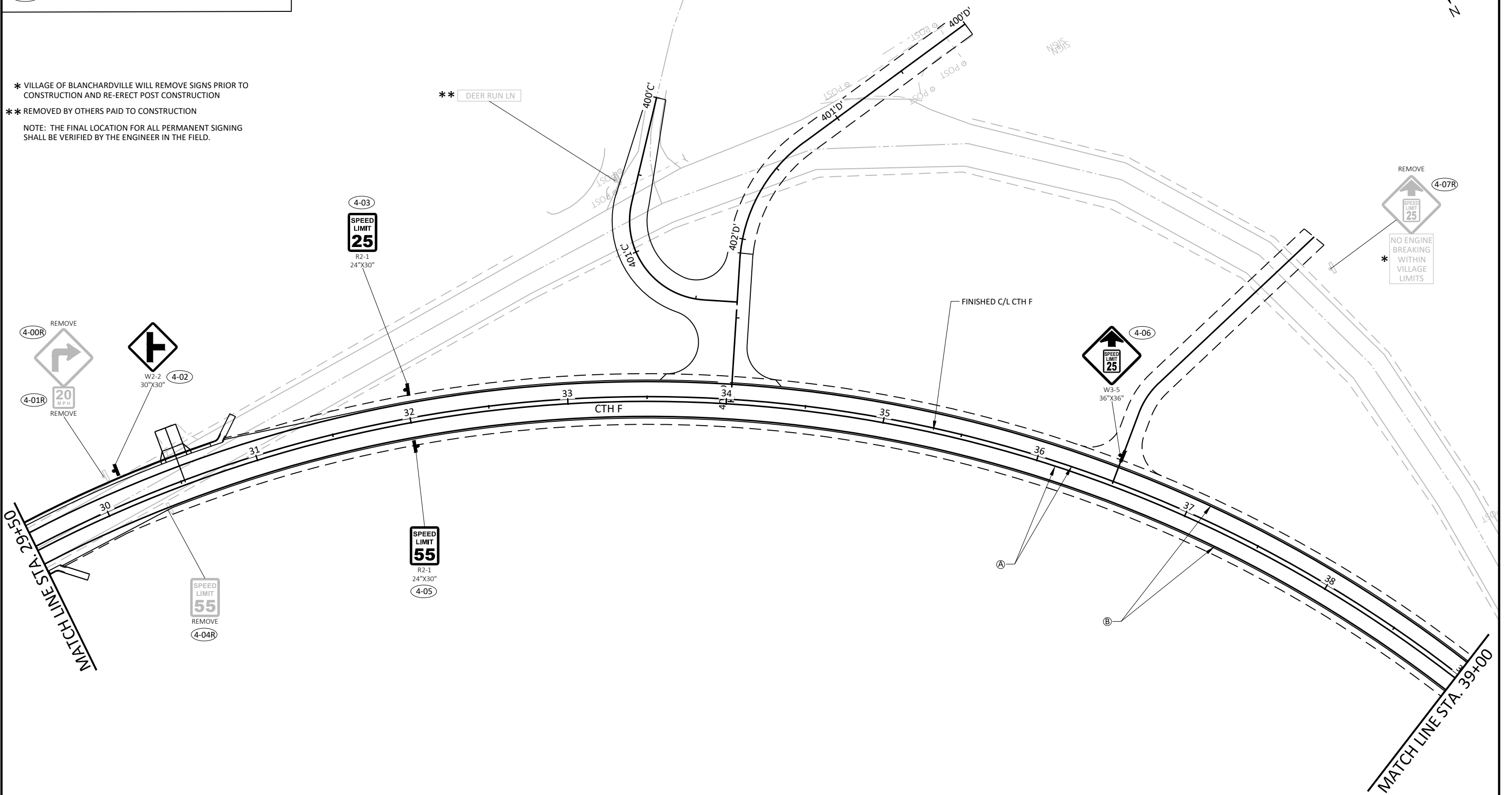
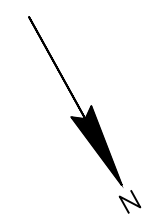
LEGEND

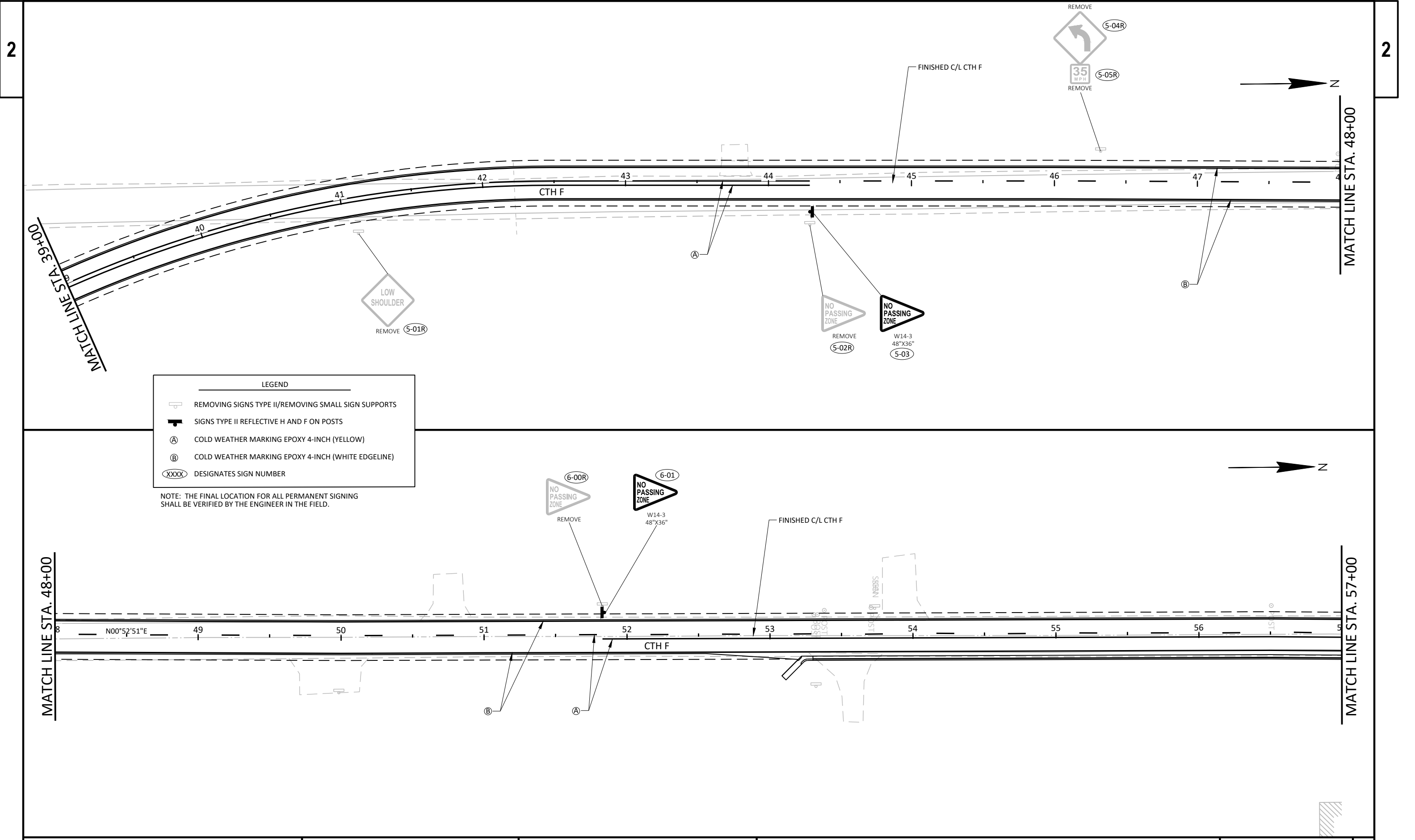
-  REMOVING SIGNS TYPE II/REMOVING SMALL SIGN SUPPORTS
-  SIGNS TYPE II REFLECTIVE H AND F ON POSTS
-  COLD WEATHER MARKING EPOXY 4-INCH (YELLOW)
-  COLD WEATHER MARKING EPOXY 4-INCH (WHITE EDGELINE)
-  DESIGNATES SIGN NUMBER

* VILLAGE OF BLANCHARDVILLE WILL REMOVE SIGNS PRIOR TO CONSTRUCTION AND RE-ERECT POST CONSTRUCTION

** REMOVED BY OTHERS PAID TO CONSTRUCTION

NOTE: THE FINAL LOCATION FOR ALL PERMANENT SIGNING SHALL BE VERIFIED BY THE ENGINEER IN THE FIELD.

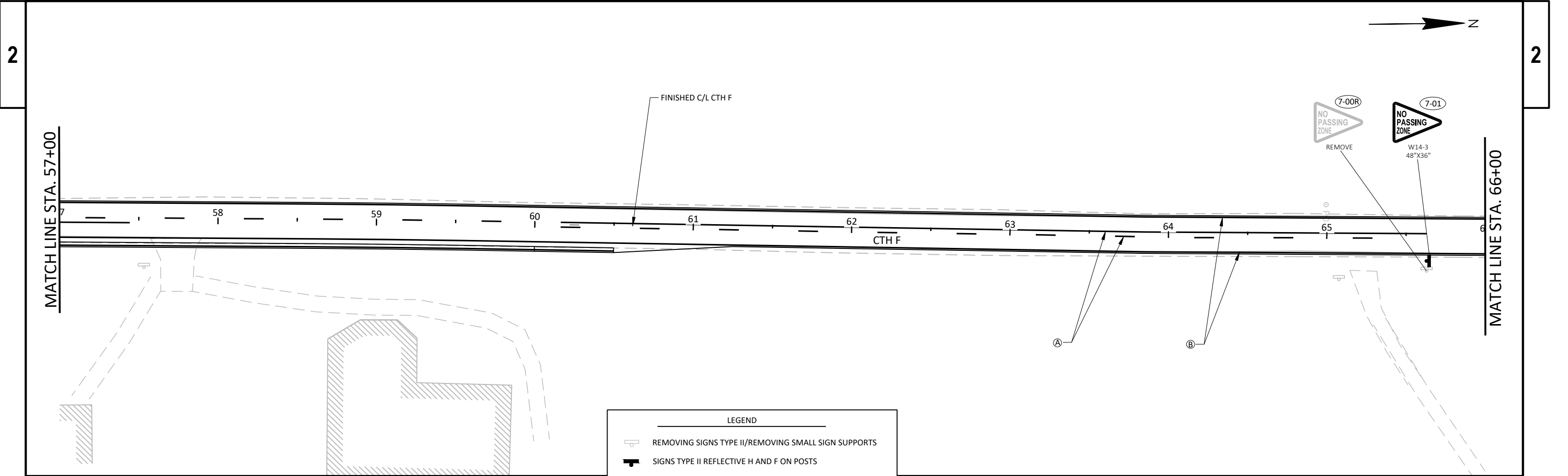









LEGEND

	REMOVING SIGNS TYPE II/REMOVING SMALL SIGN SUPPORTS
	SIGNS TYPE II REFLECTIVE H AND F ON POSTS
	COLD WEATHER MARKING EPOXY 4-INCH (YELLOW)
	COLD WEATHER MARKING EPOXY 4-INCH (WHITE EDGELINE)
	DESIGNATES SIGN NUMBER

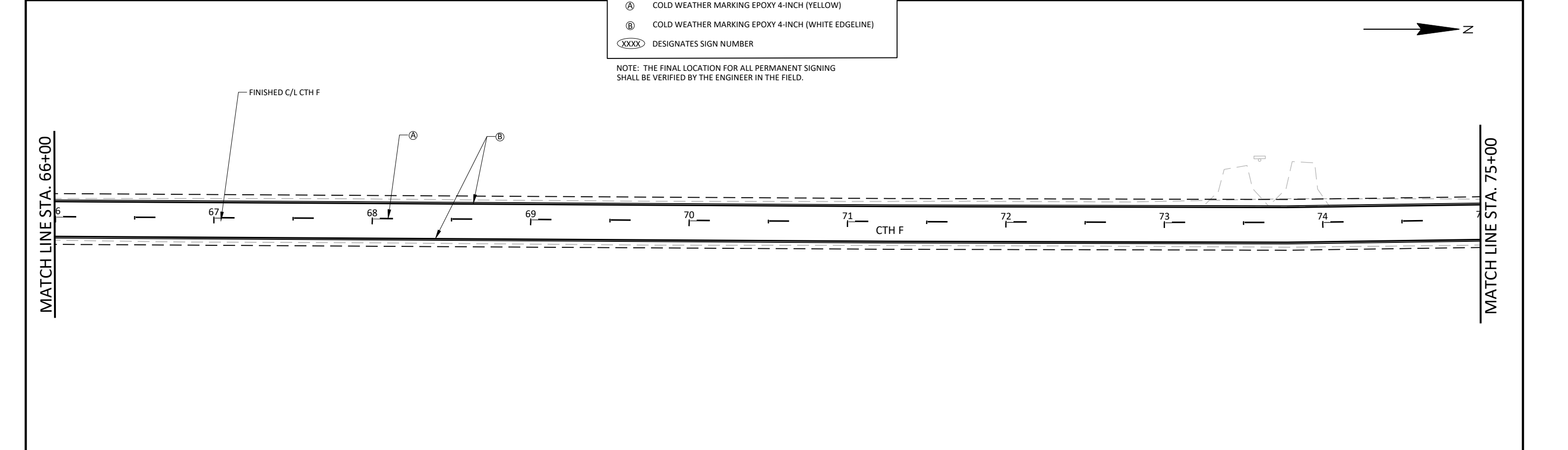
NOTE: THE FINAL LOCATION FOR ALL PERMANENT SIGNING SHALL BE VERIFIED BY THE ENGINEER IN THE FIELD.

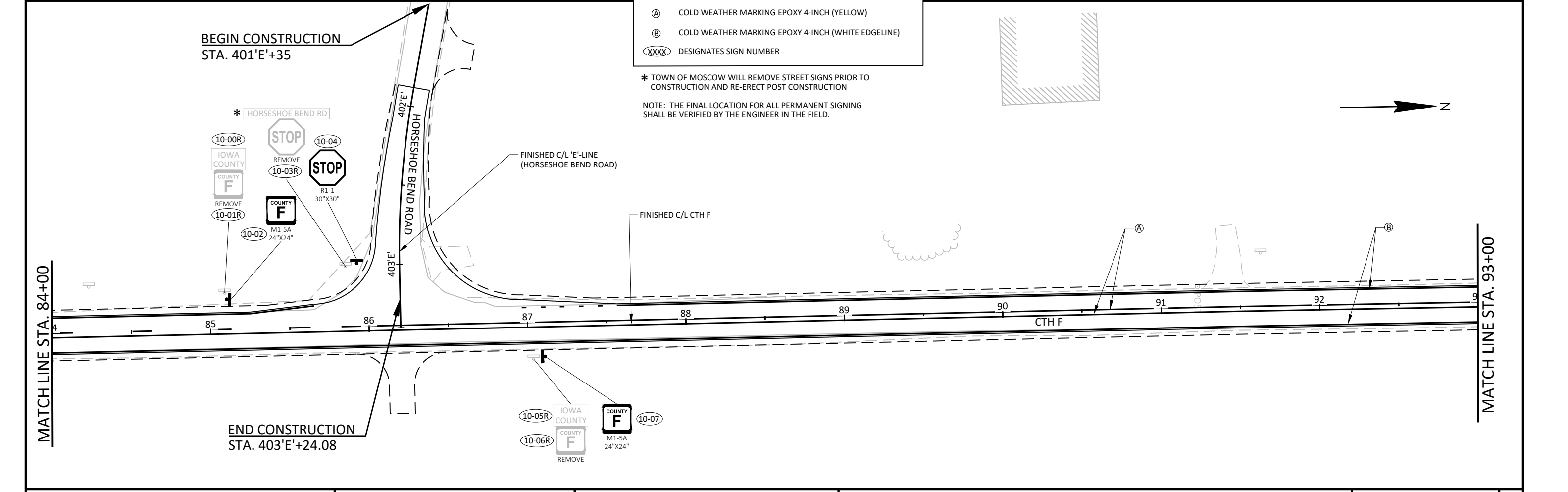
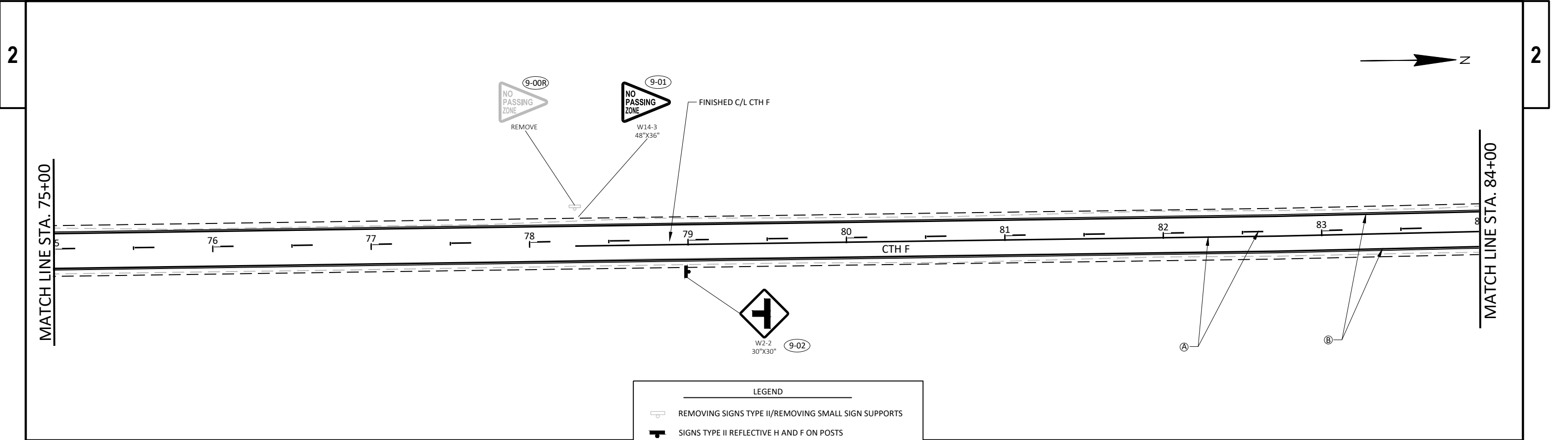


LEGEND

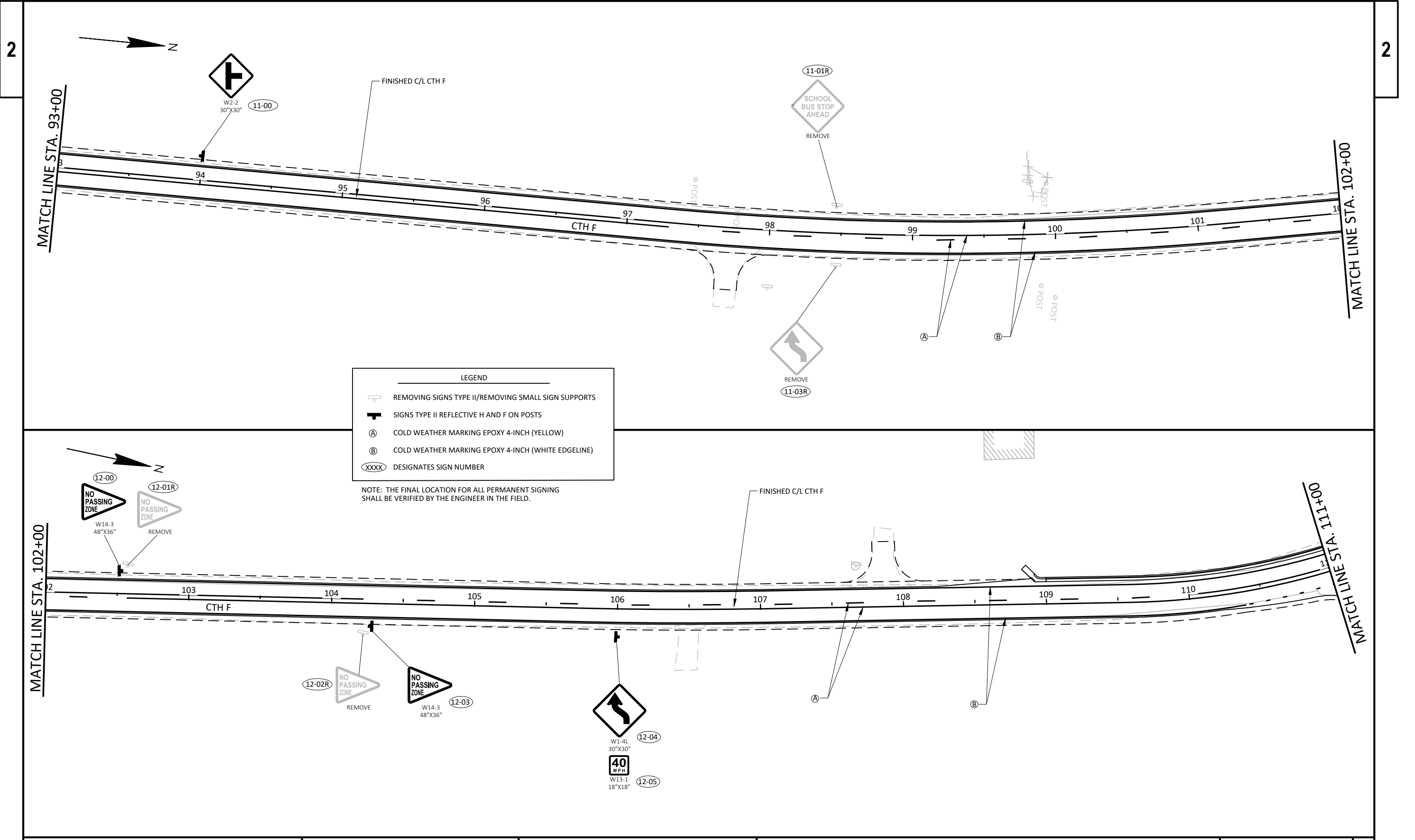
-  REMOVING SIGNS TYPE II/REMOVING SMALL SIGN SUPPORTS
-  SIGNS TYPE II REFLECTIVE H AND F ON POSTS
-  COLD WEATHER MARKING EPOXY 4-INCH (YELLOW)
-  COLD WEATHER MARKING EPOXY 4-INCH (WHITE EDGELINE)
-  DESIGNATES SIGN NUMBER

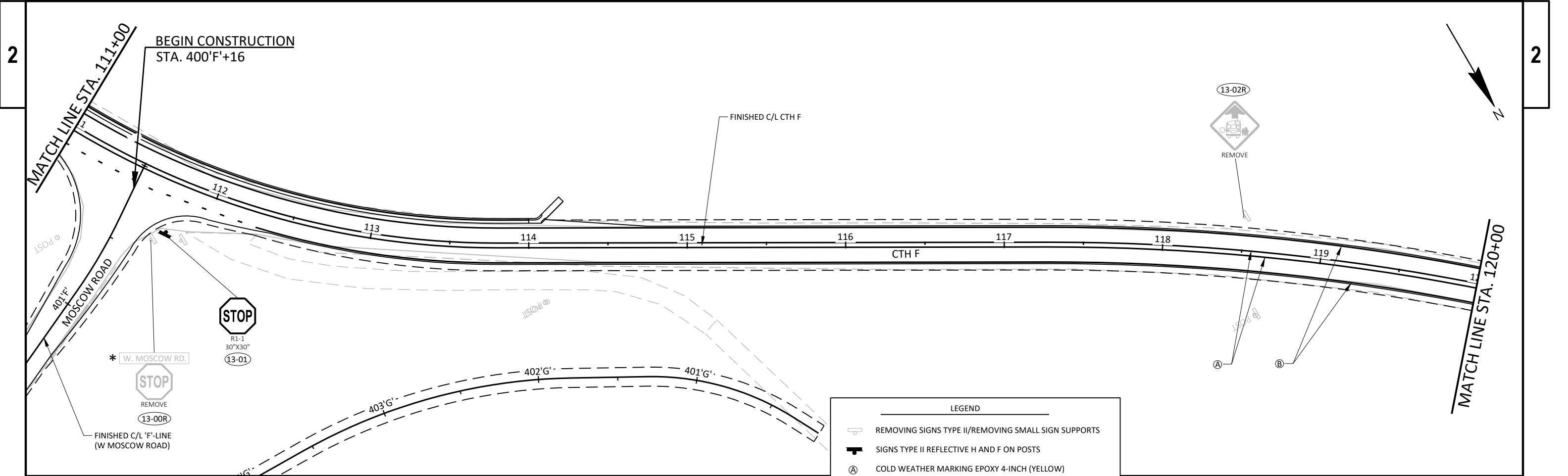
NOTE: THE FINAL LOCATION FOR ALL PERMANENT SIGNING SHALL BE VERIFIED BY THE ENGINEER IN THE FIELD.





PROJECT NO: 5034-00-72	HWY: CTH F	COUNTY: IOWA	PERMANENT SIGNING/PAVEMENT MARKING PLAN	SHEET	E
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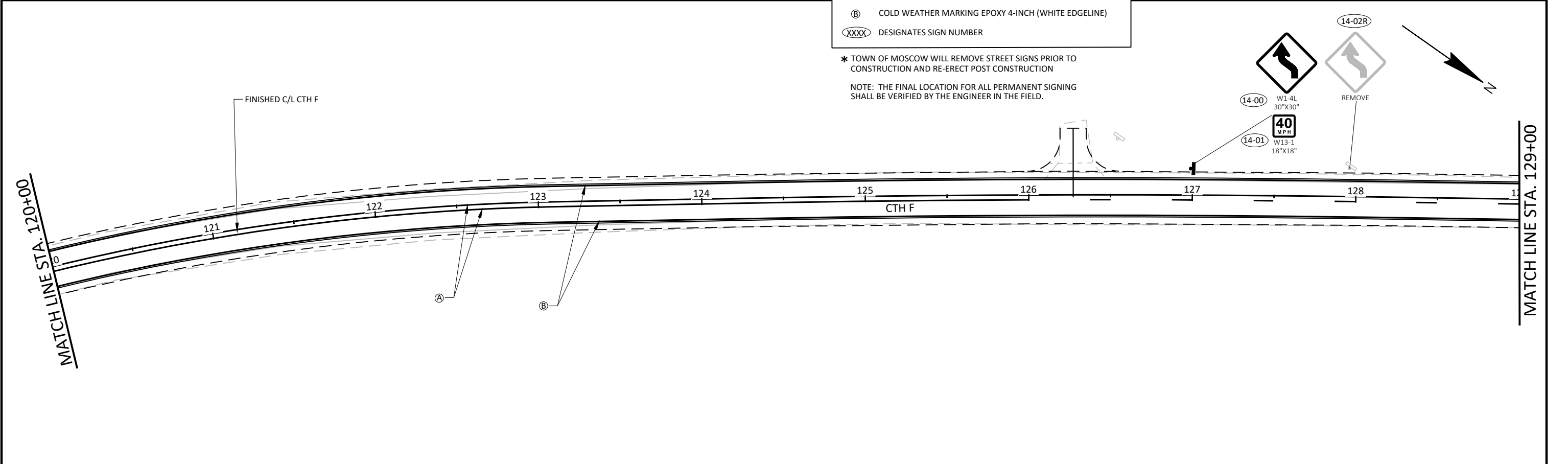


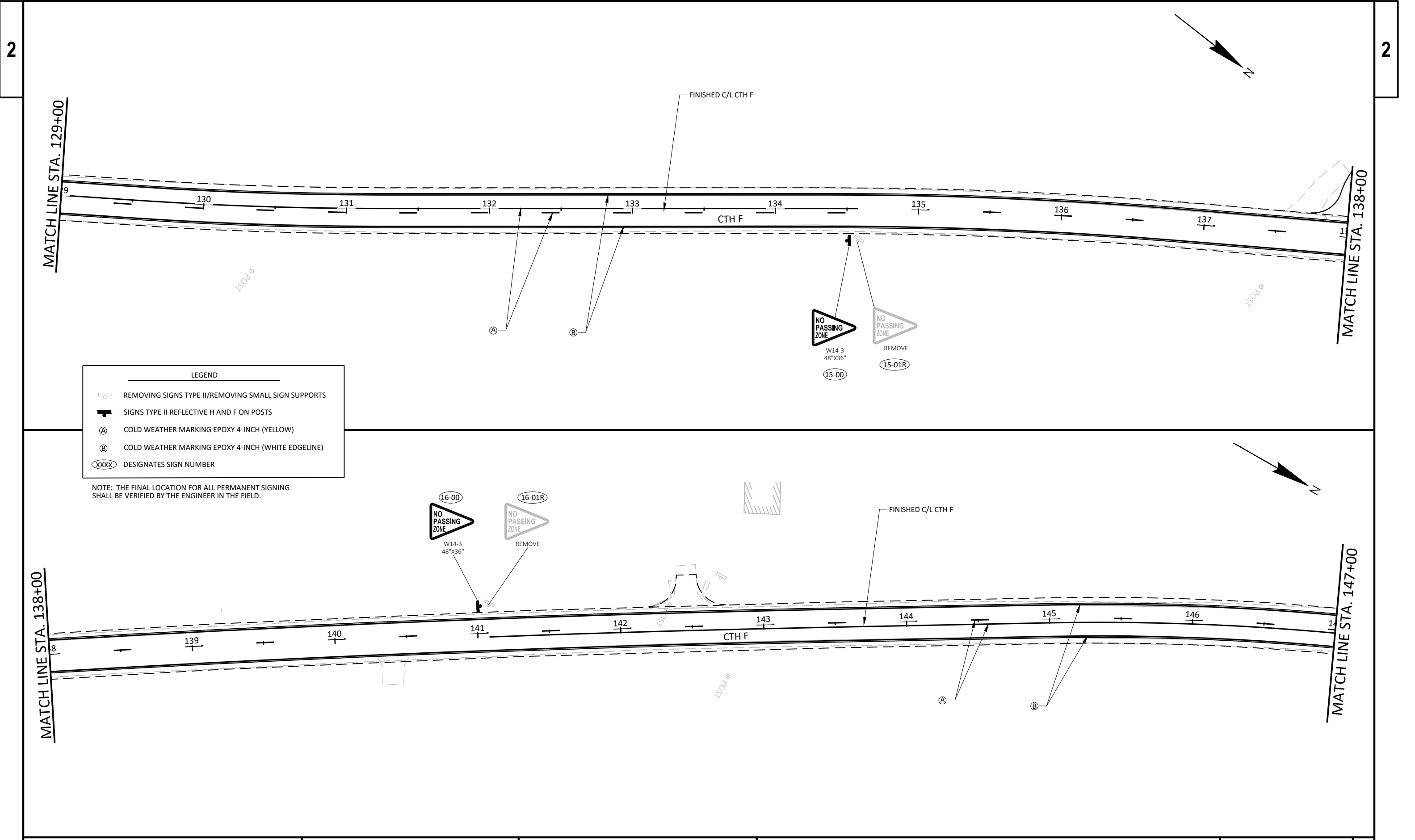


LEGEND

- REMOVING SIGNS TYPE II/REMOVING SMALL SIGN SUPPORTS
- SIGNS TYPE II REFLECTIVE H AND F ON POSTS
- COLD WEATHER MARKING EPOXY 4-INCH (YELLOW)
- COLD WEATHER MARKING EPOXY 4-INCH (WHITE EDGELINE)
- DESIGNATES SIGN NUMBER

* TOWN OF MOSCOW WILL REMOVE STREET SIGNS PRIOR TO CONSTRUCTION AND RE-ERECT POST CONSTRUCTION
 NOTE: THE FINAL LOCATION FOR ALL PERMANENT SIGNING SHALL BE VERIFIED BY THE ENGINEER IN THE FIELD.

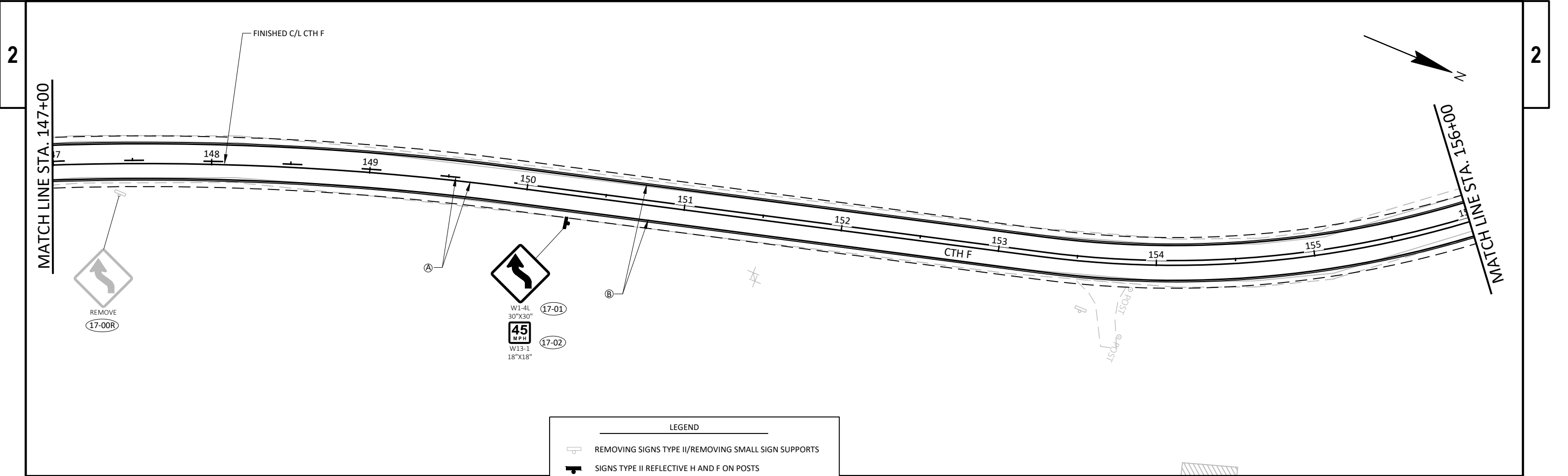









LEGEND

- REMOVING SIGNS TYPE II/REMOVING SMALL SIGN SUPPORTS
- SIGNS TYPE II REFLECTIVE H AND F ON POSTS
- COLD WEATHER MARKING EPOXY 4-INCH (YELLOW)
- COLD WEATHER MARKING EPOXY 4-INCH (WHITE EDGELINE)
- DESIGNATES SIGN NUMBER

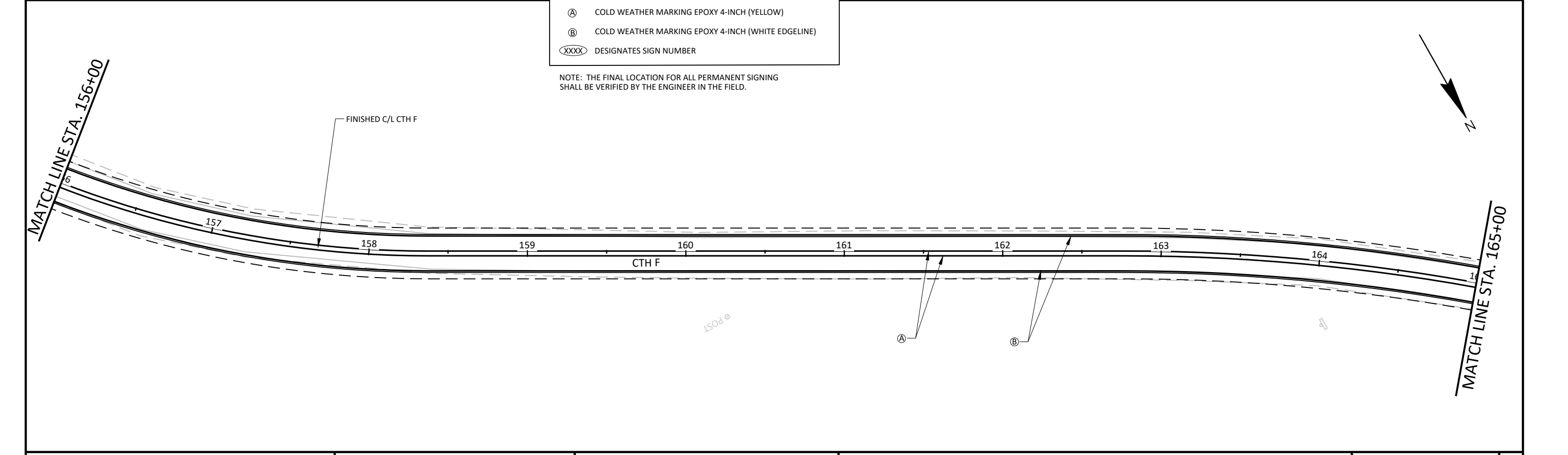
NOTE: THE FINAL LOCATION FOR ALL PERMANENT SIGNING SHALL BE VERIFIED BY THE ENGINEER IN THE FIELD.

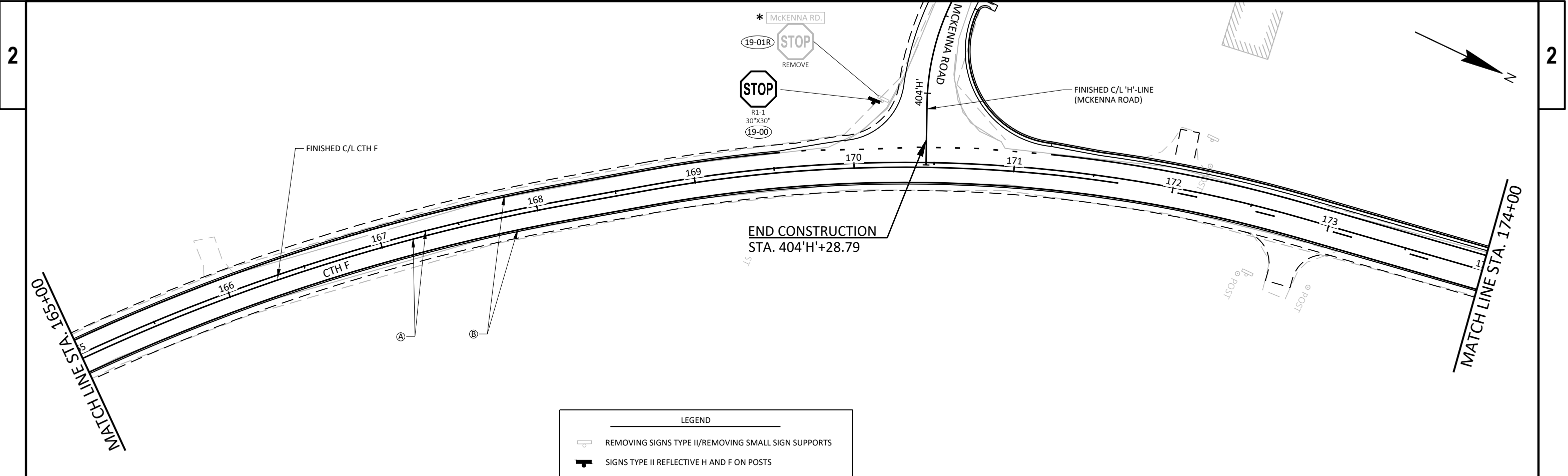


LEGEND

-  REMOVING SIGNS TYPE II/REMOVING SMALL SIGN SUPPORTS
-  SIGNS TYPE II REFLECTIVE H AND F ON POSTS
-  (A) COLD WEATHER MARKING EPOXY 4-INCH (YELLOW)
-  (B) COLD WEATHER MARKING EPOXY 4-INCH (WHITE EDGELINE)
-  (XXXX) DESIGNATES SIGN NUMBER

NOTE: THE FINAL LOCATION FOR ALL PERMANENT SIGNING SHALL BE VERIFIED BY THE ENGINEER IN THE FIELD.



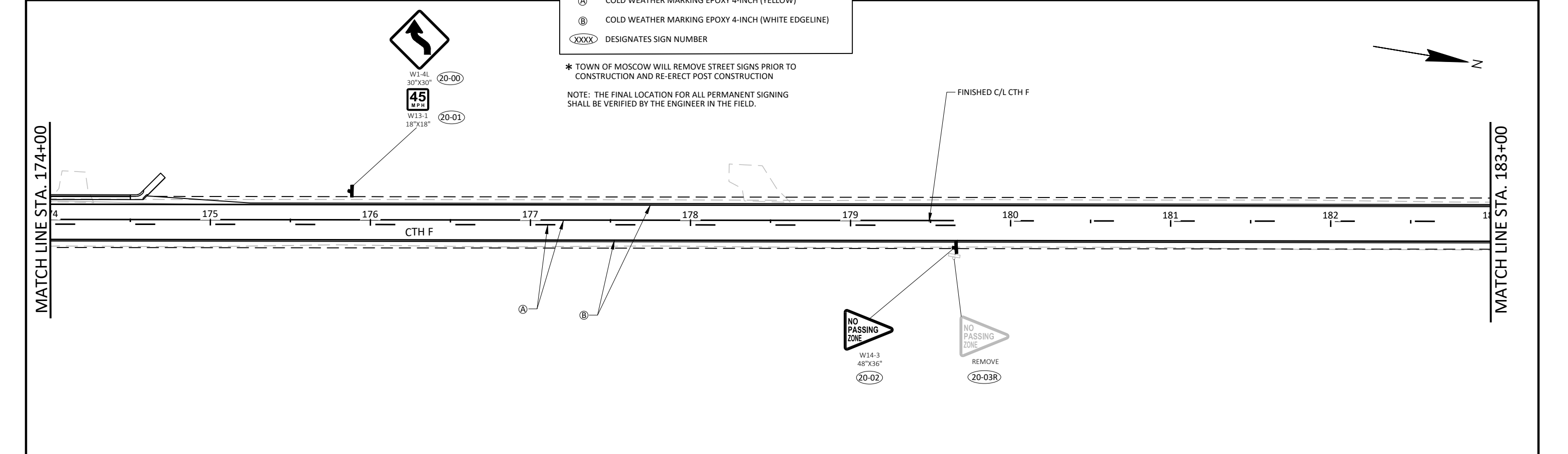


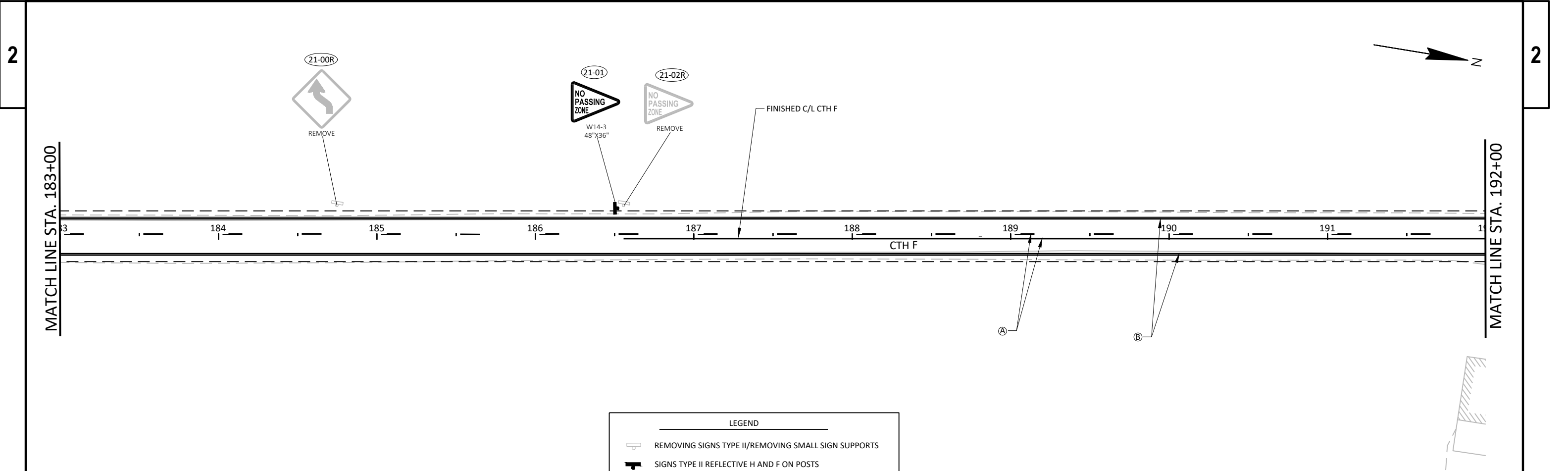
LEGEND

	REMOVING SIGNS TYPE II/REMOVING SMALL SIGN SUPPORTS
	SIGNS TYPE II REFLECTIVE H AND F ON POSTS
	COLD WEATHER MARKING EPOXY 4-INCH (YELLOW)
	COLD WEATHER MARKING EPOXY 4-INCH (WHITE EDGELINE)
	DESIGNATES SIGN NUMBER

* TOWN OF MOSCOW WILL REMOVE STREET SIGNS PRIOR TO CONSTRUCTION AND RE-ERECT POST CONSTRUCTION

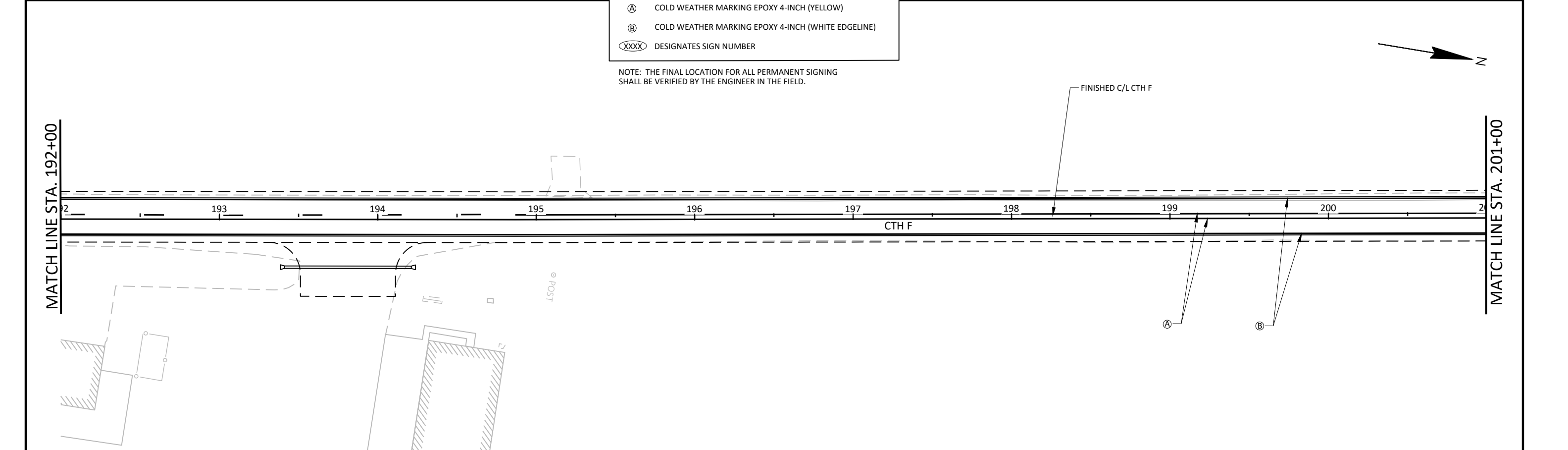
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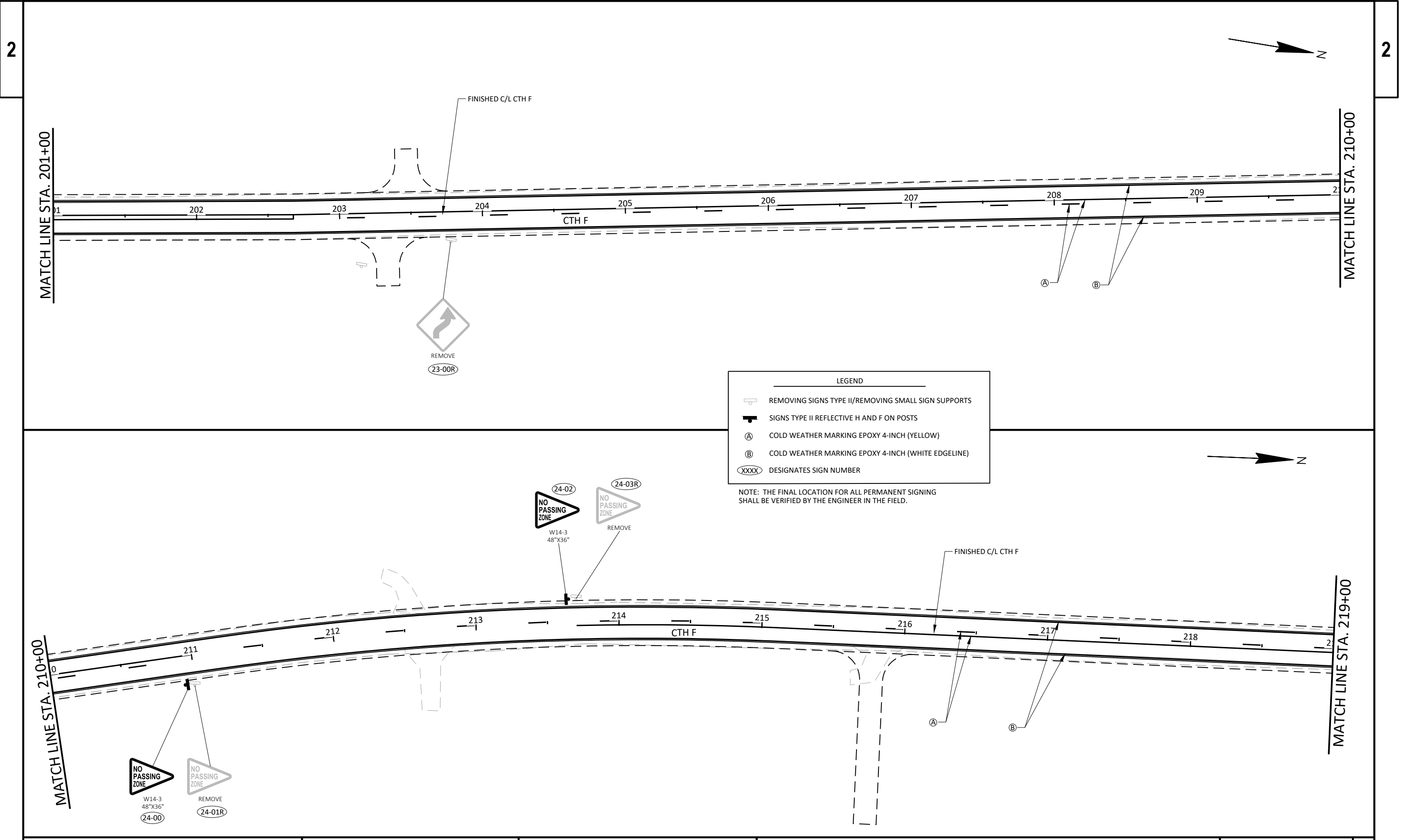









LEGEND	
	REMOVING SIGNS TYPE II/REMOVING SMALL SIGN SUPPORTS
	SIGNS TYPE II REFLECTIVE H AND F ON POSTS
	COLD WEATHER MARKING EPOXY 4-INCH (YELLOW)
	COLD WEATHER MARKING EPOXY 4-INCH (WHITE EDGELINE)
	DESIGNATES SIGN NUMBER

NOTE: THE FINAL LOCATION FOR ALL PERMANENT SIGNING SHALL BE VERIFIED BY THE ENGINEER IN THE FIELD.

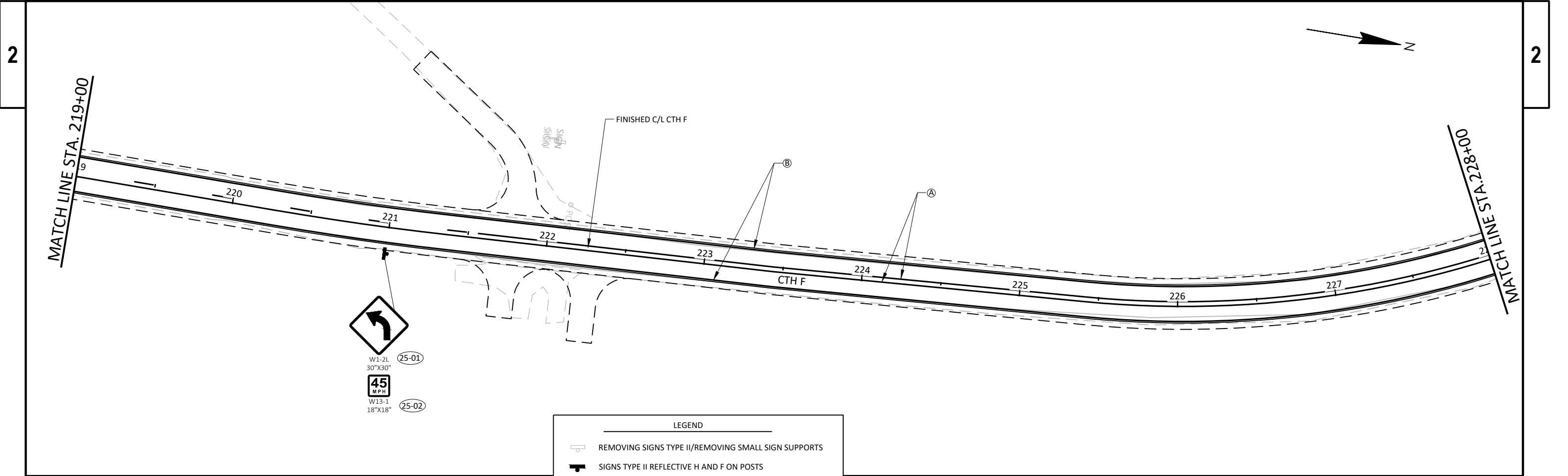




LEGEND

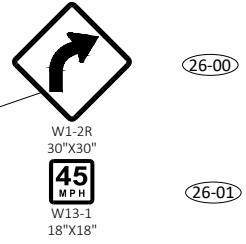
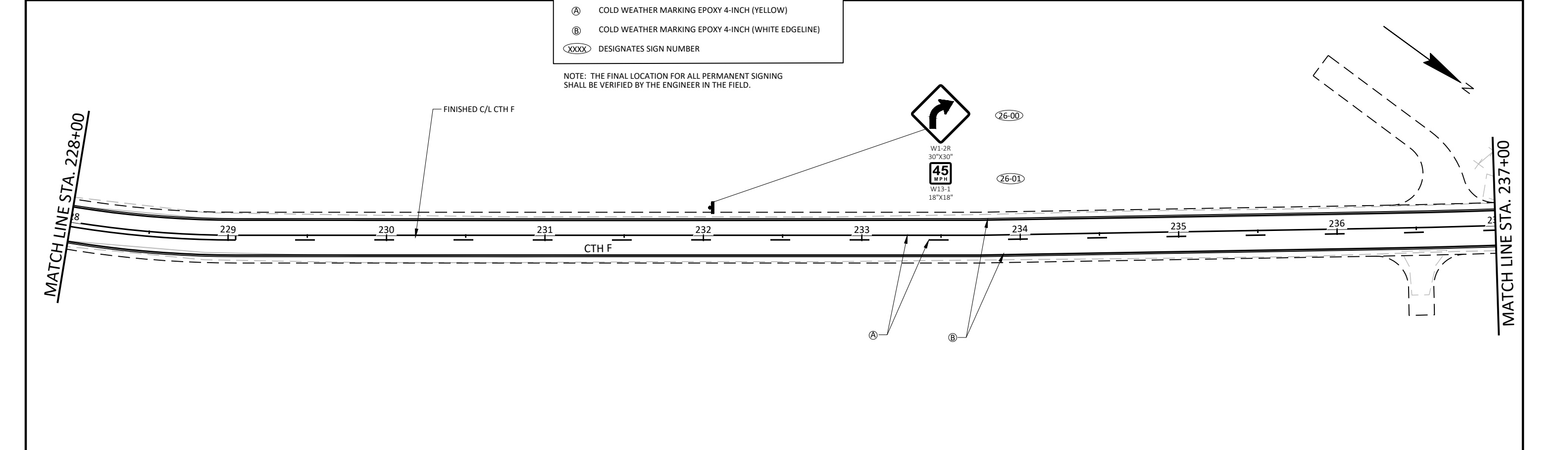
-  REMOVING SIGNS TYPE II/REMOVING SMALL SIGN SUPPORTS
-  SIGNS TYPE II REFLECTIVE H AND F ON POSTS
-  COLD WEATHER MARKING EPOXY 4-INCH (YELLOW)
-  COLD WEATHER MARKING EPOXY 4-INCH (WHITE EDGELINE)
-  DESIGNATES SIGN NUMBER

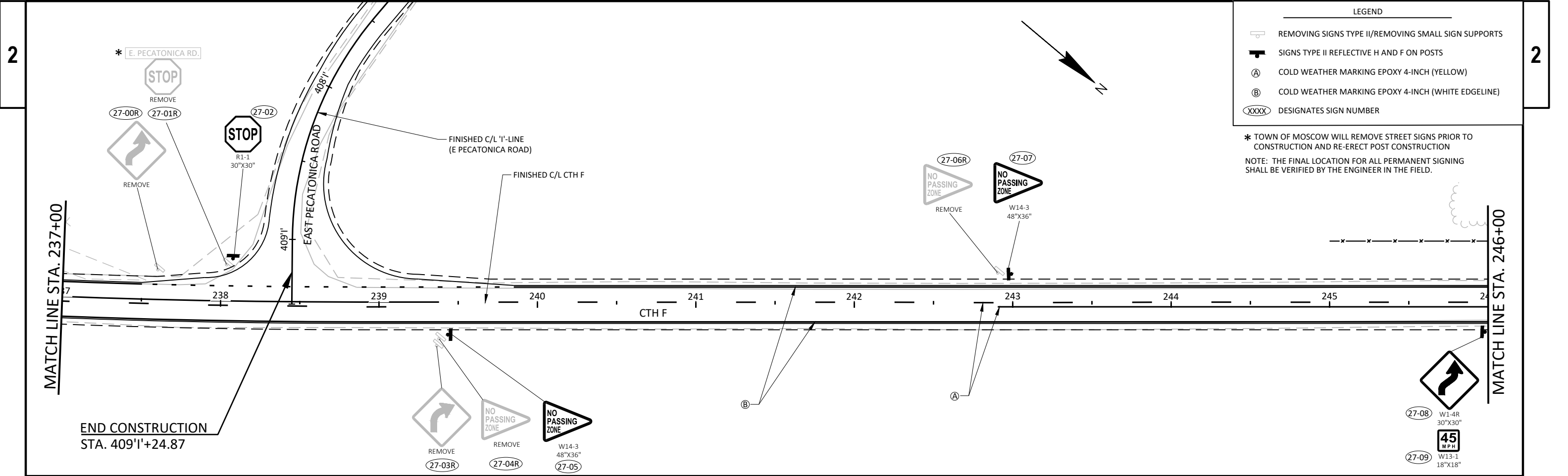
NOTE: THE FINAL LOCATION FOR ALL PERMANENT SIGNING SHALL BE VERIFIED BY THE ENGINEER IN THE FIELD.



LEGEND	
	REMOVING SIGNS TYPE II/REMOVING SMALL SIGN SUPPORTS
	SIGNS TYPE II REFLECTIVE H AND F ON POSTS
	COLD WEATHER MARKING EPOXY 4-INCH (YELLOW)
	COLD WEATHER MARKING EPOXY 4-INCH (WHITE EDGELINE)
	DESIGNATES SIGN NUMBER

NOTE: THE FINAL LOCATION FOR ALL PERMANENT SIGNING SHALL BE VERIFIED BY THE ENGINEER IN THE FIELD.

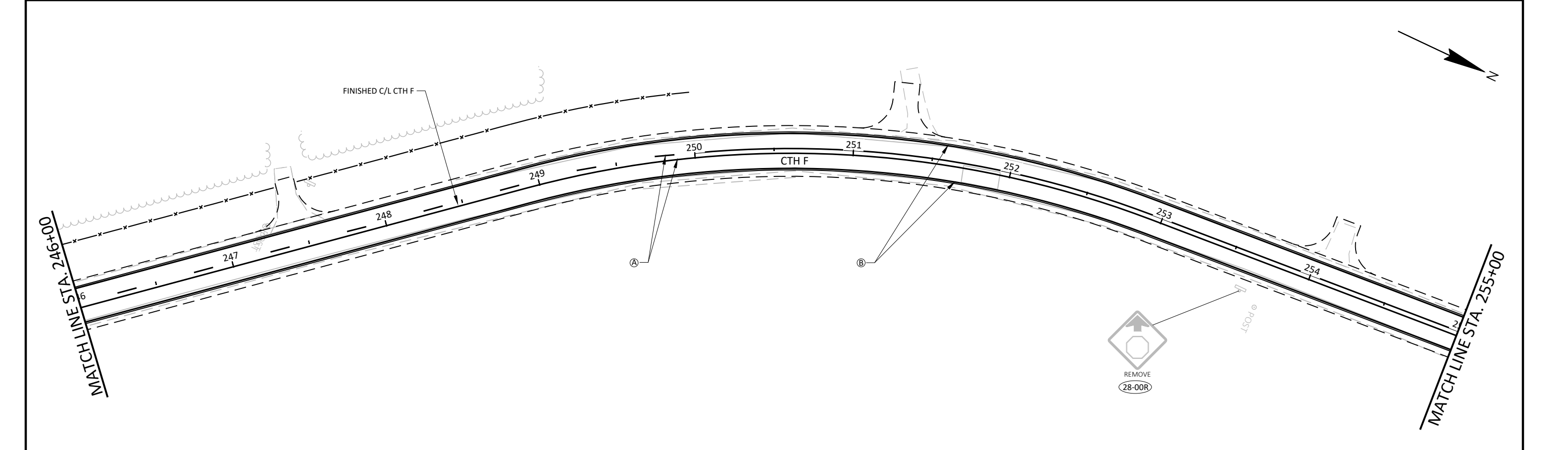




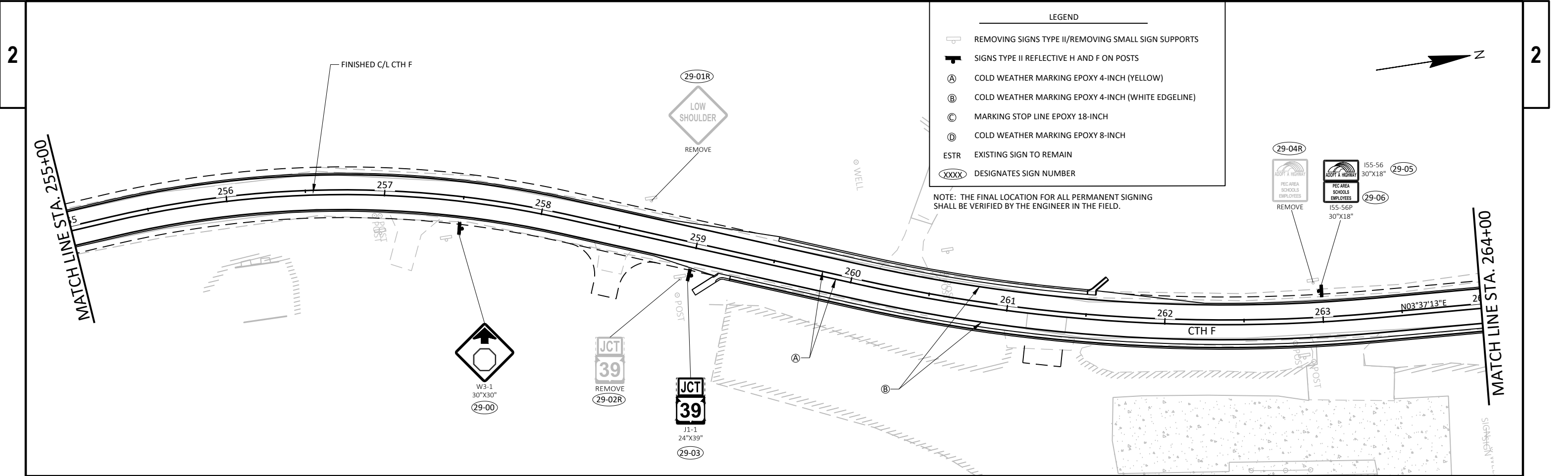
LEGEND

- REMOVING SIGNS TYPE II/REMOVING SMALL SIGN SUPPORTS
- SIGNS TYPE II REFLECTIVE H AND F ON POSTS
- COLD WEATHER MARKING EPOXY 4-INCH (YELLOW)
- COLD WEATHER MARKING EPOXY 4-INCH (WHITE EDGELINE)
- DESIGNATES SIGN NUMBER

* TOWN OF MOSCOW WILL REMOVE STREET SIGNS PRIOR TO CONSTRUCTION AND RE-ERECT POST CONSTRUCTION
 NOTE: THE FINAL LOCATION FOR ALL PERMANENT SIGNING SHALL BE VERIFIED BY THE ENGINEER IN THE FIELD.



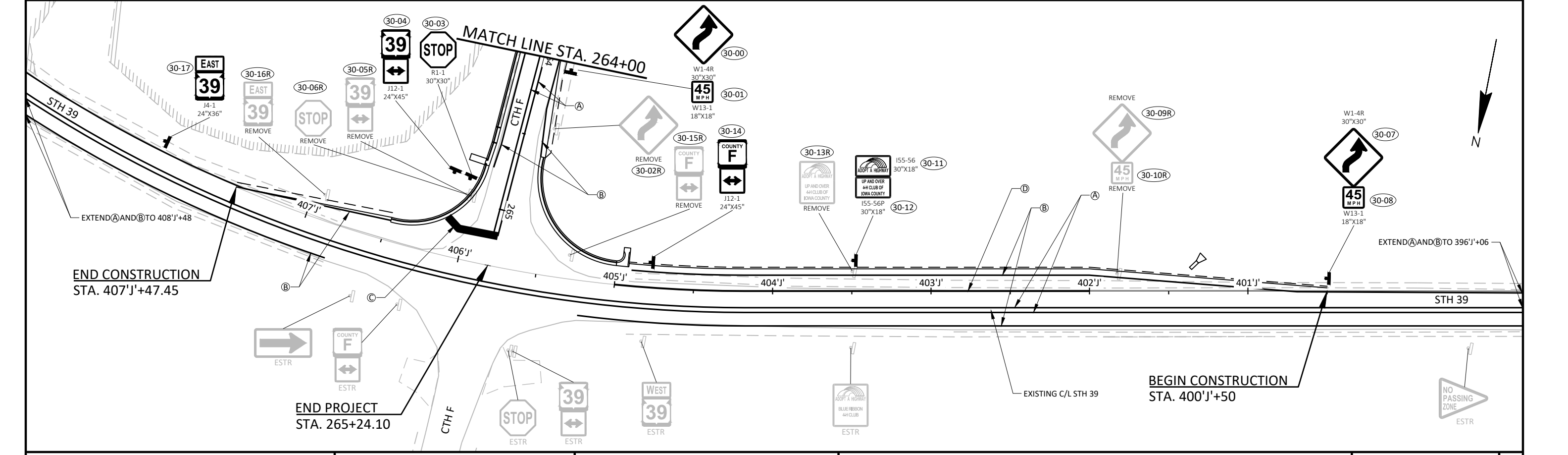
PROJECT NO: 5034-00-72	HWY: CTH F	COUNTY: IOWA	PERMANENT SIGNING/PAVEMENT MARKING PLAN	SHEET	E
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LEGEND

- REMOVING SIGNS TYPE II/REMOVING SMALL SIGN SUPPORTS
- SIGNS TYPE II REFLECTIVE H AND F ON POSTS
- COLD WEATHER MARKING EPOXY 4-INCH (YELLOW)
- COLD WEATHER MARKING EPOXY 4-INCH (WHITE EDGELINE)
- MARKING STOP LINE EPOXY 18-INCH
- COLD WEATHER MARKING EPOXY 8-INCH
- EXISTING SIGN TO REMAIN
- DESIGNATES SIGN NUMBER

NOTE: THE FINAL LOCATION FOR ALL PERMANENT SIGNING SHALL BE VERIFIED BY THE ENGINEER IN THE FIELD.



PROJECT NO: 5034-00-72	HWY: CTH F	COUNTY: IOWA	PERMANENT SIGNING/PAVEMENT MARKING PLAN	SHEET	E
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TRAFFIC CONTROL GENERAL NOTES

THE EXACT LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

ALL SIGNS INAPPROPRIATE TO THE STATUS OF THE CONTROL ZONE, INCLUDING PRE-EXISTING SIGNING IN THE VICINITY, SHALL BE COVERED OR REMOVED AS DIRECTED BY THE ENGINEER.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS, EXCEPT THE BACKGROUND IS ORANGE.

ALL "W" AND "WO" SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED.

ANY STOP SIGNS WHICH ARE REMOVED FOR A CONSTRUCTION OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED.

ALL SIGN LAYOUT SHALL BE IN ACCORDANCE WITH THE FEDERAL HIGHWAY ADMINISTRATION MANUAL OF STANDARD HIGHWAY SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

THE CONTRACTOR SHALL PROVIDE AND MAINTAIN ALL NECESSARY BARRICADES, SIGNS, LIGHTS, TEMPORARY MARKINGS, FLAGGERS AND SUCH OTHER SAFETY DEVICES AS CALLED FOR ON THE PLANS OR AS DIRECTED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200' CLEARANCE TO EXISTING SIGNS.

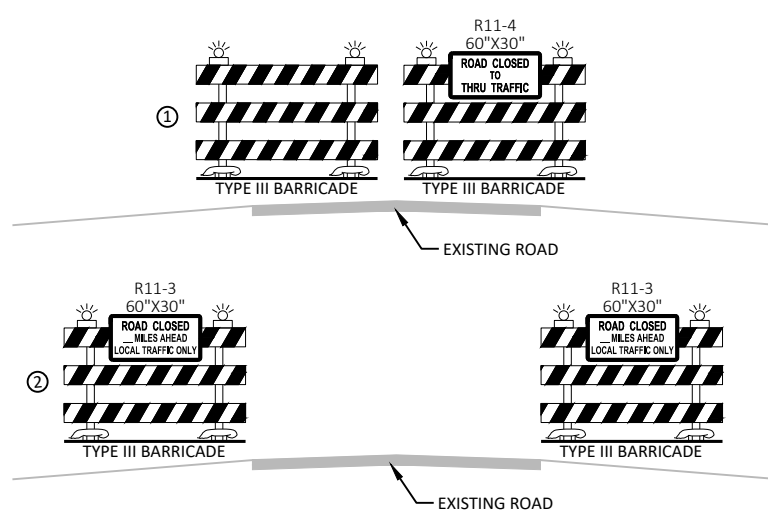
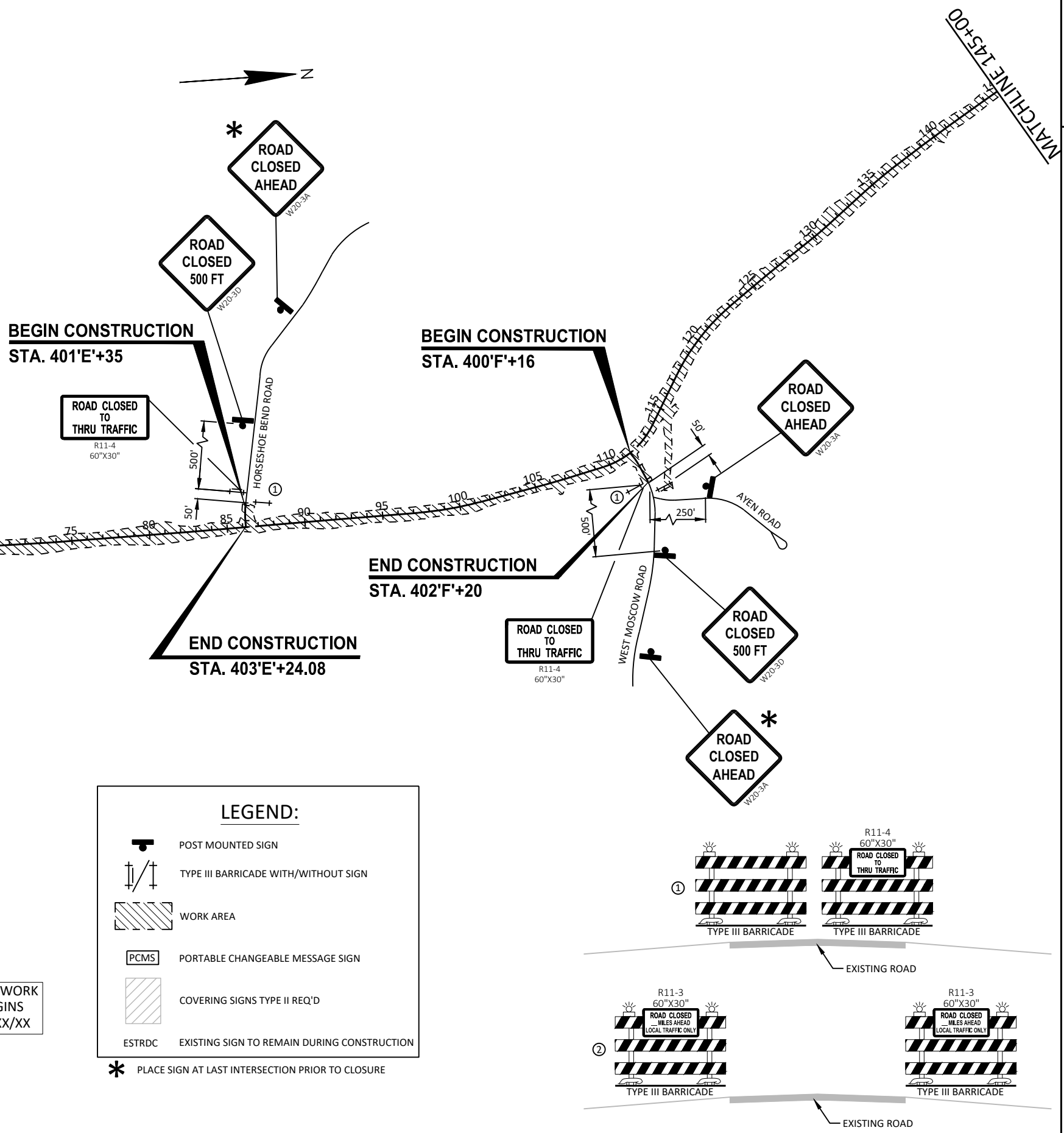
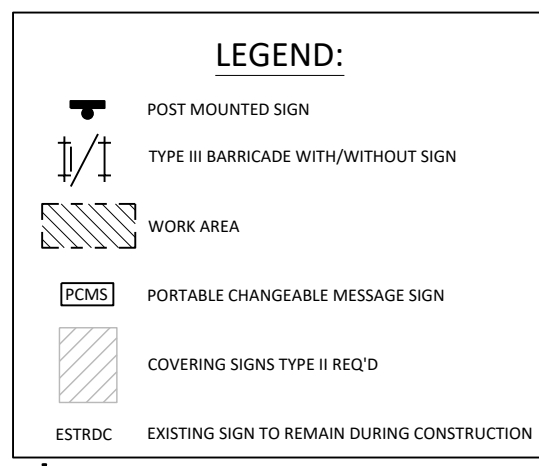
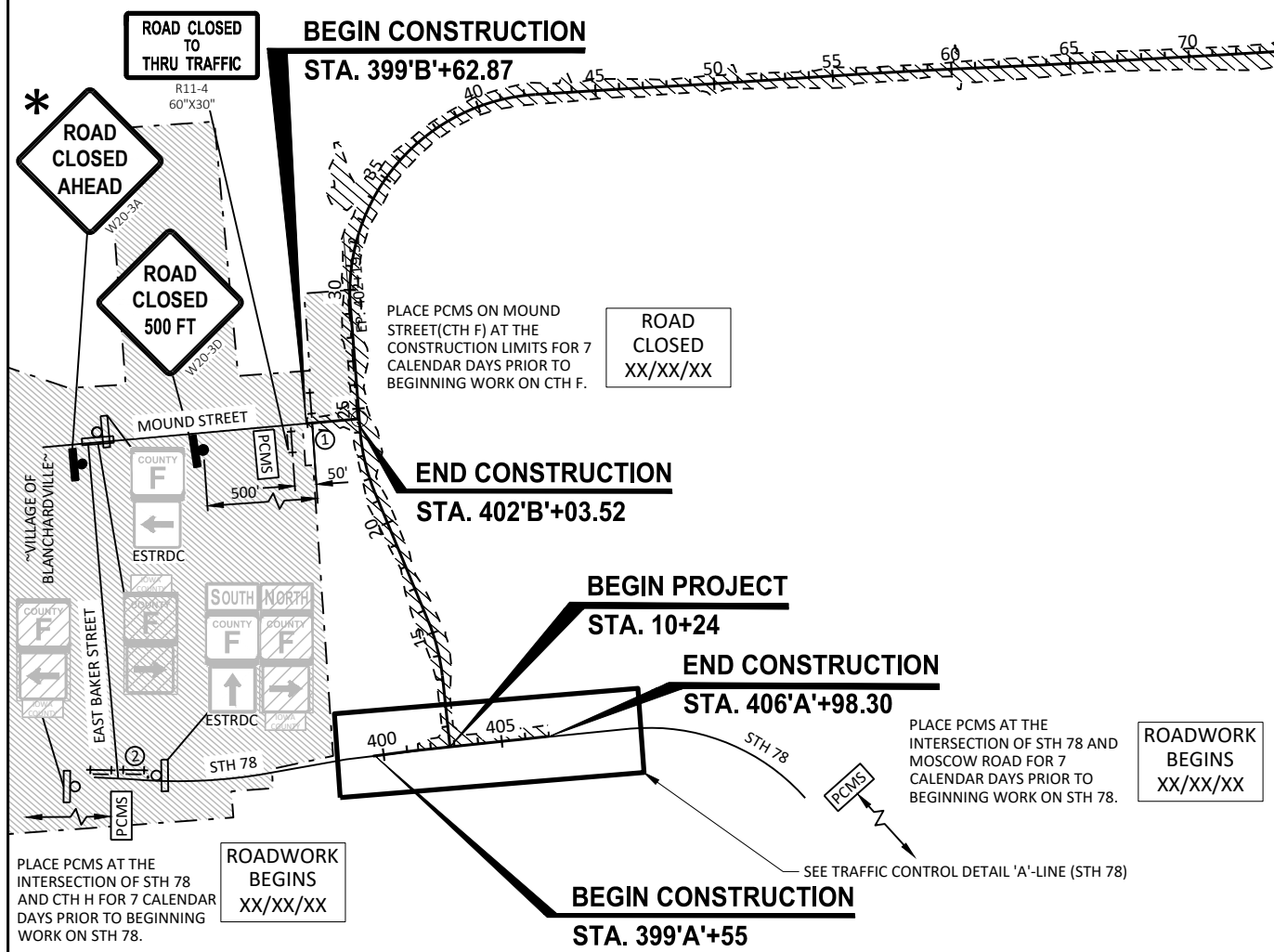
DURING HOURS OF DARKNESS, ALL BARRICADES USED TO SHIELD A HAZARD SHALL BE EQUIPPED WITH TYPE "A" (LOW INTENSITY FLASHING) LIGHTS.

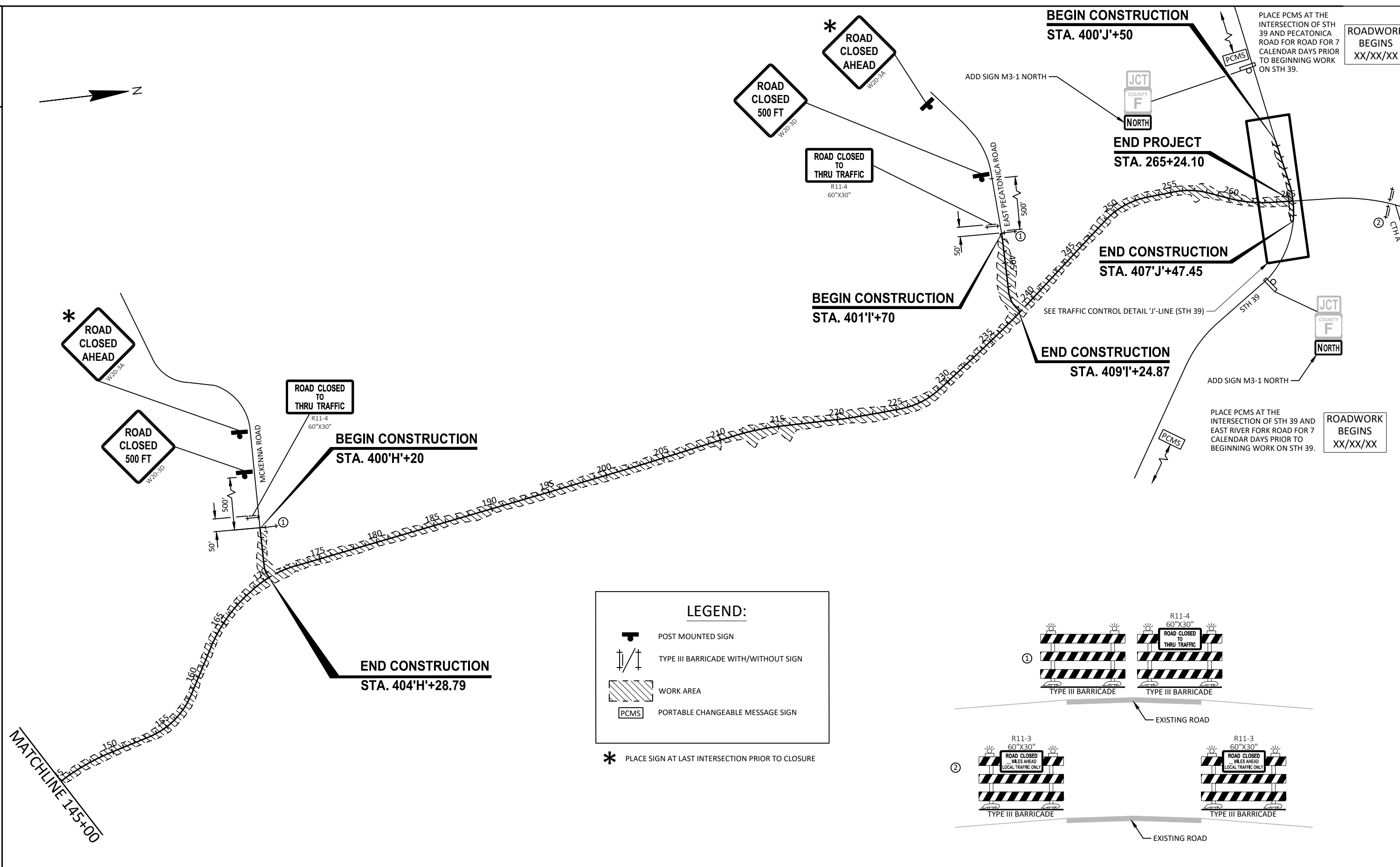
DRAWINGS SHOW TRAFFIC CONTROL FOR A TYPICAL SITUATION. ADDITIONAL TRAFFIC CONTROL DEVICES MAY BE REQUIRED AND/OR LAYOUT DETAILS MODIFIED DEPENDING ON CONTRACTOR'S METHODS OR SEQUENCES OF OPERATION.

ROAD MACHINERY, FLAGGERS AHEAD, ETC. SIGNS SHALL BE USED AS NEEDED AND SHALL BE REMOVED OR COVERED AT NIGHTS, ON WEEKENDS OR WHEN THE ACTIVITY DOES NOT EXIST.

ADDITIONAL DRUMS OR TYPE III BARRICADES MAY BE REQUIRED ADJACENT TO DROP-OFFS, OPEN TRENCHES, OR PROTRUSIONS. COST TO BE INCLUDED WITH OPERATION WHICH CREATES THE HAZARD.

DRUMS PLACED ADJACENT TO WORK AREAS SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

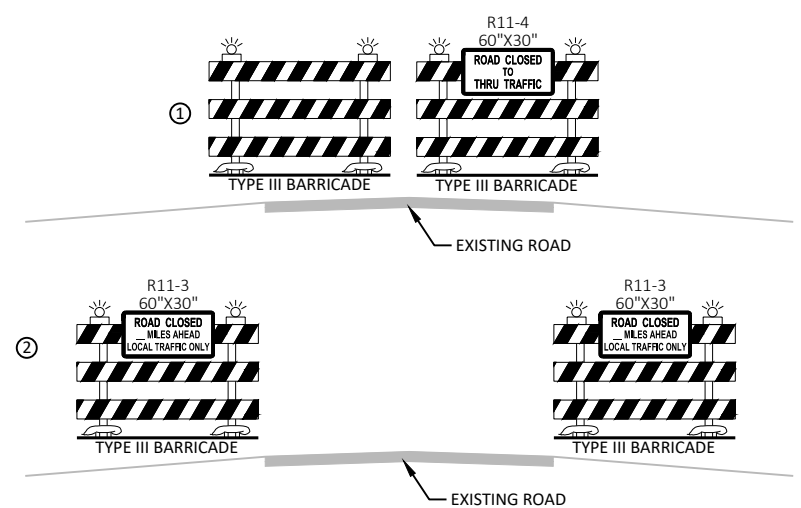




LEGEND:

- POST MOUNTED SIGN
- TYPE III BARRICADE WITH/WITHOUT SIGN
- WORK AREA
- PORTABLE CHANGEABLE MESSAGE SIGN

* PLACE SIGN AT LAST INTERSECTION PRIOR TO CLOSURE

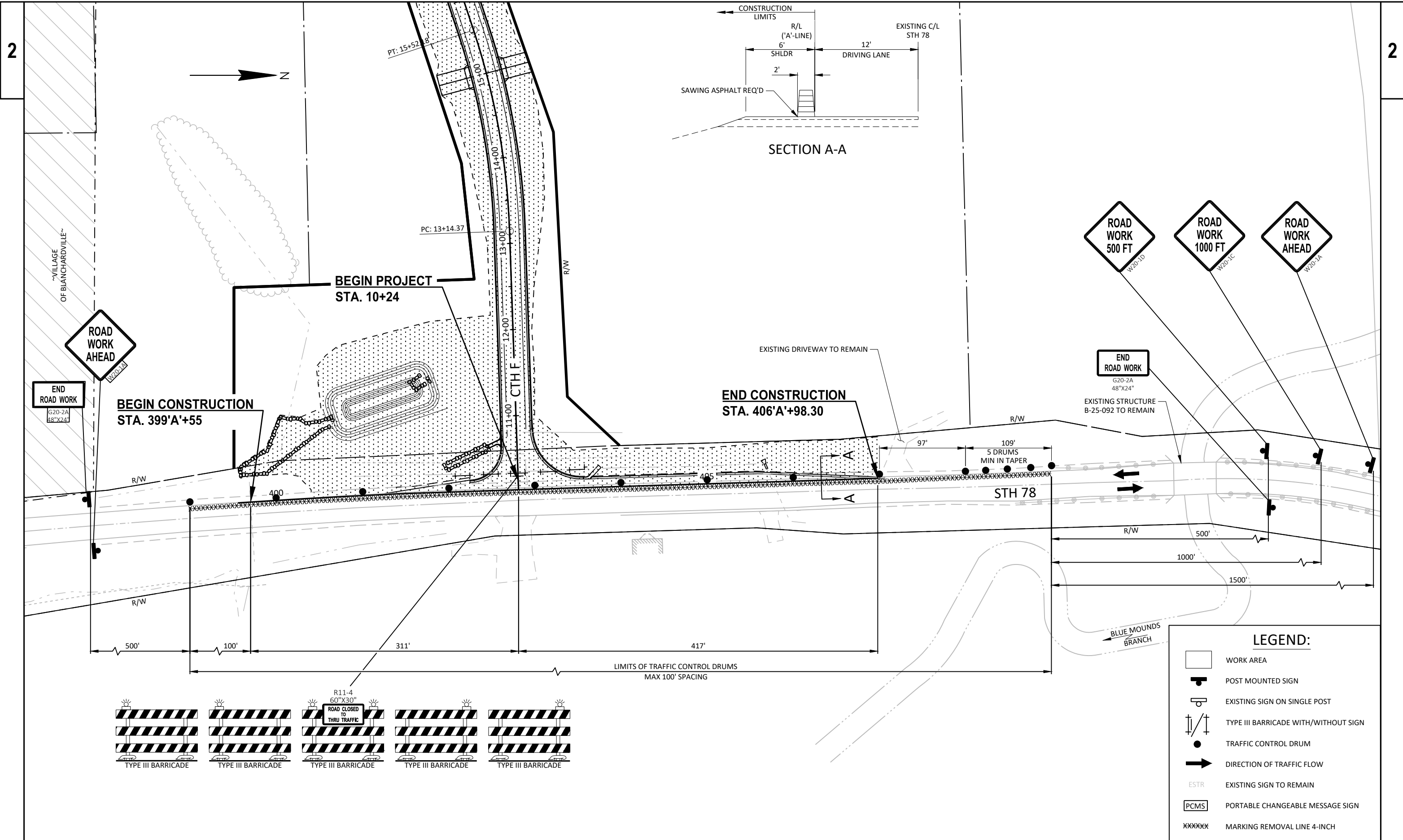


PLACE PCMS AT THE INTERSECTION OF STH 39 AND PECATONICA ROAD FOR ROAD FOR 7 CALENDAR DAYS PRIOR TO BEGINNING WORK ON STH 39.











ROADWORK BEGINS XX/XX/XX

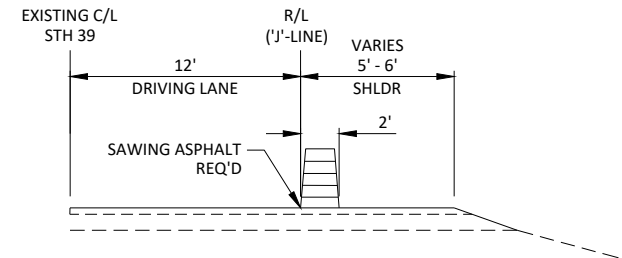
PLACE PCMS AT THE INTERSECTION OF STH 39 AND EAST RIVER FORK ROAD FOR 7 CALENDAR DAYS PRIOR TO BEGINNING WORK ON STH 39.

ROADWORK BEGINS XX/XX/XX

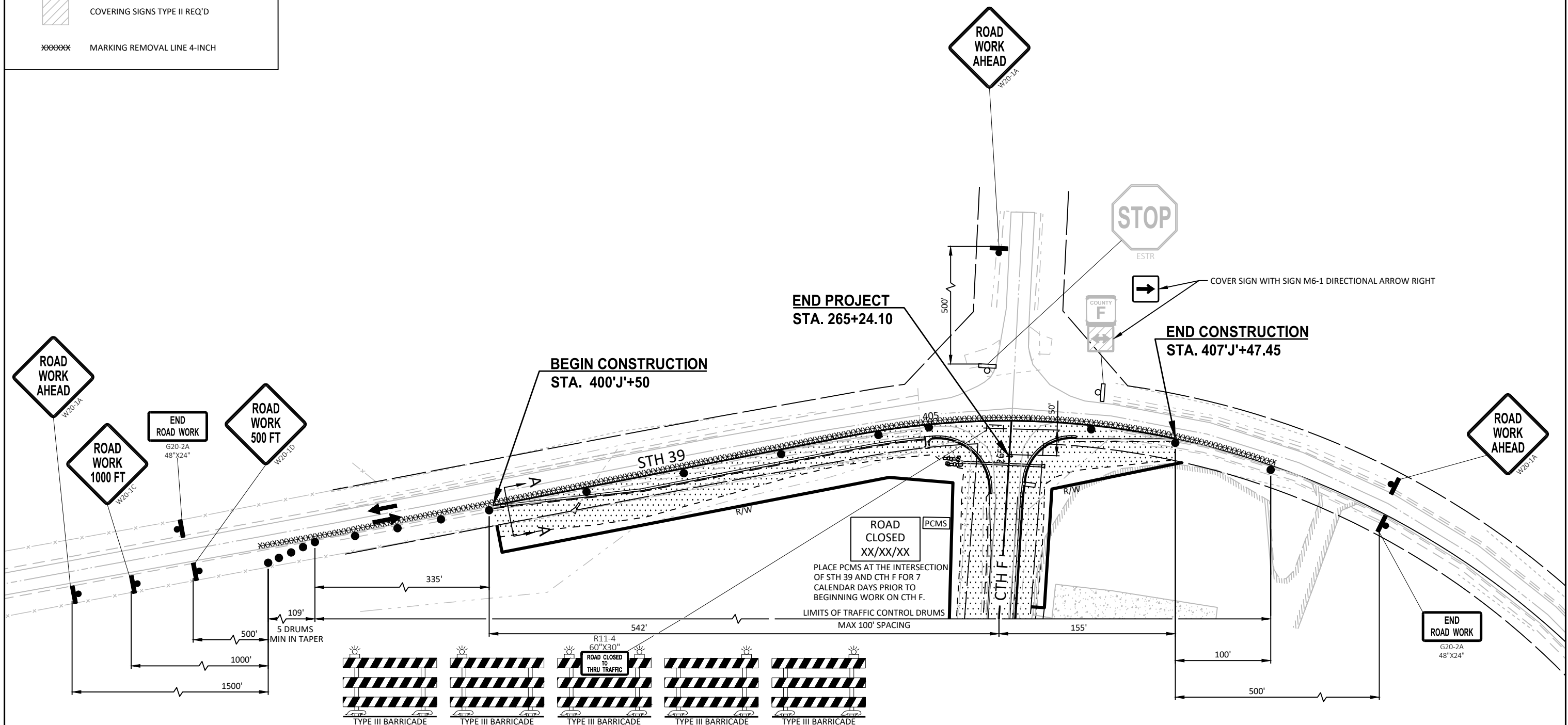


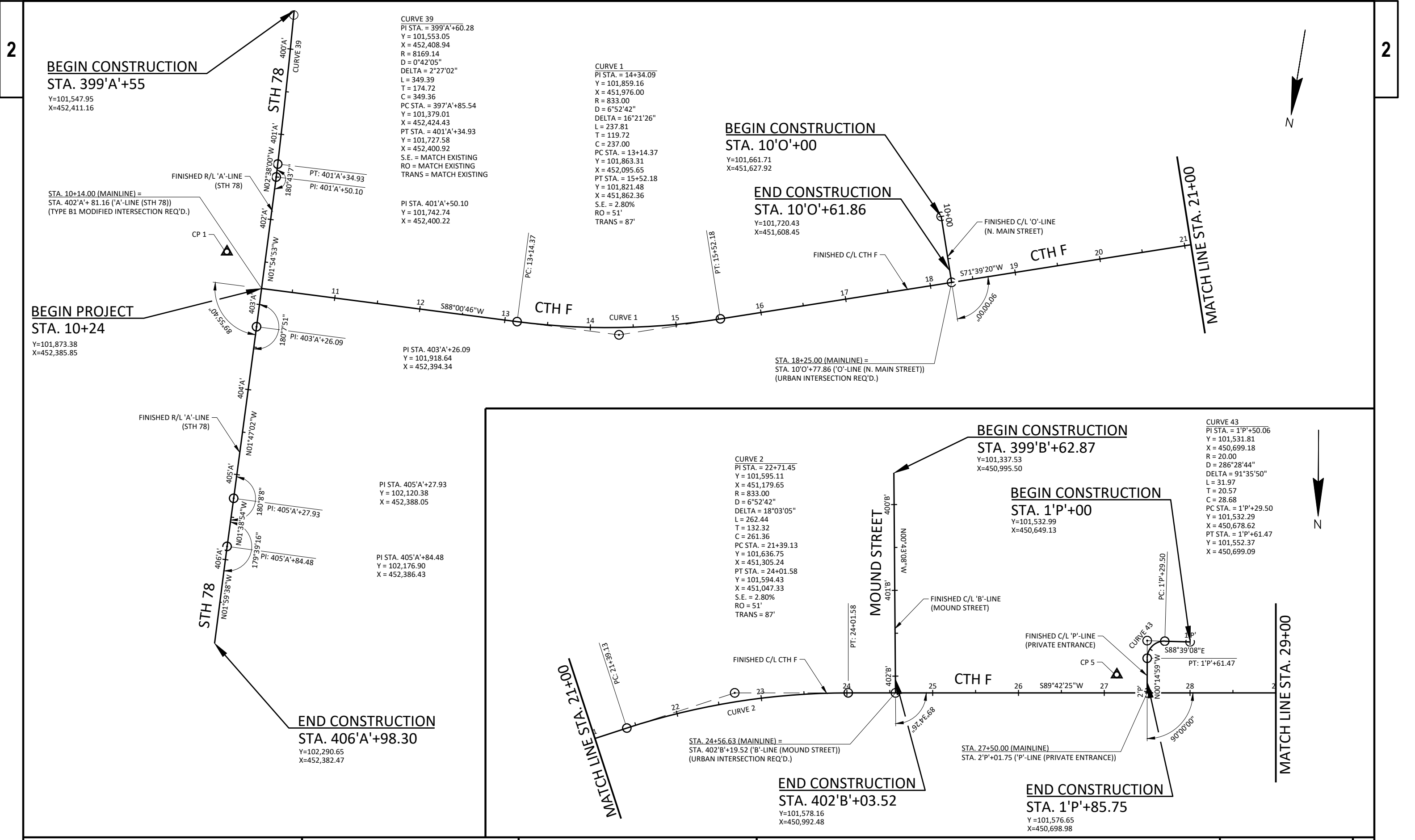
LEGEND:

-  WORK AREA
-  POST MOUNTED SIGN
-  EXISTING SIGN ON SINGLE POST
-  TYPE III BARRICADE WITH/WITHOUT SIGN
-  TRAFFIC CONTROL DRUM
-  DIRECTION OF TRAFFIC FLOW
-  EXISTING SIGN TO REMAIN
-  PORTABLE CHANGEABLE MESSAGE SIGN
-  COVERING SIGNS TYPE II REQ'D
-  MARKING REMOVAL LINE 4-INCH



SECTION A-A





BEGIN CONSTRUCTION
STA. 399'A'+55

Y=101,547.95
 X=452,411.16

STA. 10+14.00 (MAINLINE) =
 STA. 402'A'+ 81.16 ('A'-LINE (STH 78))
 (TYPE B1 MODIFIED INTERSECTION REQ'D.)

BEGIN PROJECT
STA. 10+24

Y=101,873.38
 X=452,385.85

CURVE 39
 PI STA. = 399'A'+60.28
 Y = 101,553.05
 X = 452,408.94
 R = 8169.14
 D = 0°42'05"
 DELTA = 2°27'02"
 L = 349.39
 T = 174.72
 C = 349.36
 PC STA. = 397'A'+85.54
 Y = 101,379.01
 X = 452,424.43
 PT STA. = 401'A'+34.93
 Y = 101,727.58
 X = 452,400.92
 S.E. = MATCH EXISTING
 RO = MATCH EXISTING
 TRANS = MATCH EXISTING

CURVE 1
 PI STA. = 14+34.09
 Y = 101,859.16
 X = 451,976.00
 R = 833.00
 D = 6°52'42"
 DELTA = 16°21'26"
 L = 237.81
 T = 119.72
 C = 237.00
 PC STA. = 13+14.37
 Y = 101,863.31
 X = 452,095.65
 PT STA. = 15+52.18
 Y = 101,821.48
 X = 451,862.36
 S.E. = 2.80%
 RO = 51'
 TRANS = 87'

BEGIN CONSTRUCTION
STA. 10'O'+00

Y=101,661.71
 X=451,627.92

END CONSTRUCTION
STA. 10'O'+61.86

Y=101,720.43
 X=451,608.45

STA. 18+25.00 (MAINLINE) =
 STA. 10'O'+77.86 ('O'-LINE (N. MAIN STREET))
 (URBAN INTERSECTION REQ'D.)

FINISHED R/L 'A'-LINE
 (STH 78)

FINISHED R/L 'A'-LINE
 (STH 78)

FINISHED R/L 'A'-LINE
 (STH 78)

END CONSTRUCTION
STA. 406'A'+98.30

Y=102,290.65
 X=452,382.47

PI STA. 405'A'+27.93
 Y = 102,120.38
 X = 452,388.05

PI STA. 405'A'+84.48
 Y = 102,176.90
 X = 452,386.43

CURVE 2
 PI STA. = 22+71.45
 Y = 101,595.11
 X = 451,179.65
 R = 833.00
 D = 6°52'42"
 DELTA = 18°03'05"
 L = 262.44
 T = 132.32
 C = 261.36
 PC STA. = 21+39.13
 Y = 101,636.75
 X = 451,305.24
 PT STA. = 24+01.58
 Y = 101,594.43
 X = 451,047.33
 S.E. = 2.80%
 RO = 51'
 TRANS = 87'

BEGIN CONSTRUCTION
STA. 399'B'+62.87

Y=101,337.53
 X=450,995.50

BEGIN CONSTRUCTION
STA. 1'P'+00

Y=101,532.99
 X=450,649.13

CURVE 43
 PI STA. = 1'P'+50.06
 Y = 101,531.81
 X = 450,699.18
 R = 20.00
 D = 286°28'44"
 DELTA = 91°35'50"
 L = 31.97
 T = 20.57
 C = 28.68
 PC STA. = 1'P'+29.50
 Y = 101,532.29
 X = 450,678.62
 PT STA. = 1'P'+61.47
 Y = 101,552.37
 X = 450,699.09

MOUND STREET

FINISHED C/L 'B'-LINE
 (MOUND STREET)

FINISHED C/L 'P'-LINE
 (PRIVATE ENTRANCE)

MATCH LINE STA. 21+00

STA. 24+56.63 (MAINLINE) =
 STA. 402'B'+19.52 ('B'-LINE (MOUND STREET))
 (URBAN INTERSECTION REQ'D.)

STA. 27+50.00 (MAINLINE)
 STA. 2'P'+01.75 ('P'-LINE (PRIVATE ENTRANCE))

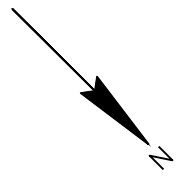
END CONSTRUCTION
STA. 402'B'+03.52

Y=101,578.16
 X=450,992.48

END CONSTRUCTION
STA. 1'P'+85.75

Y=101,576.65
 X=450,698.98

MATCH LINE STA. 29+00



BEGIN CONSTRUCTION
STA. 400'C'+00
 Y=101,531.71
 X=450,019.32

BEGIN CONSTRUCTION
STA. 400'D'+00
 Y=101,585.11
 X=449,826.47

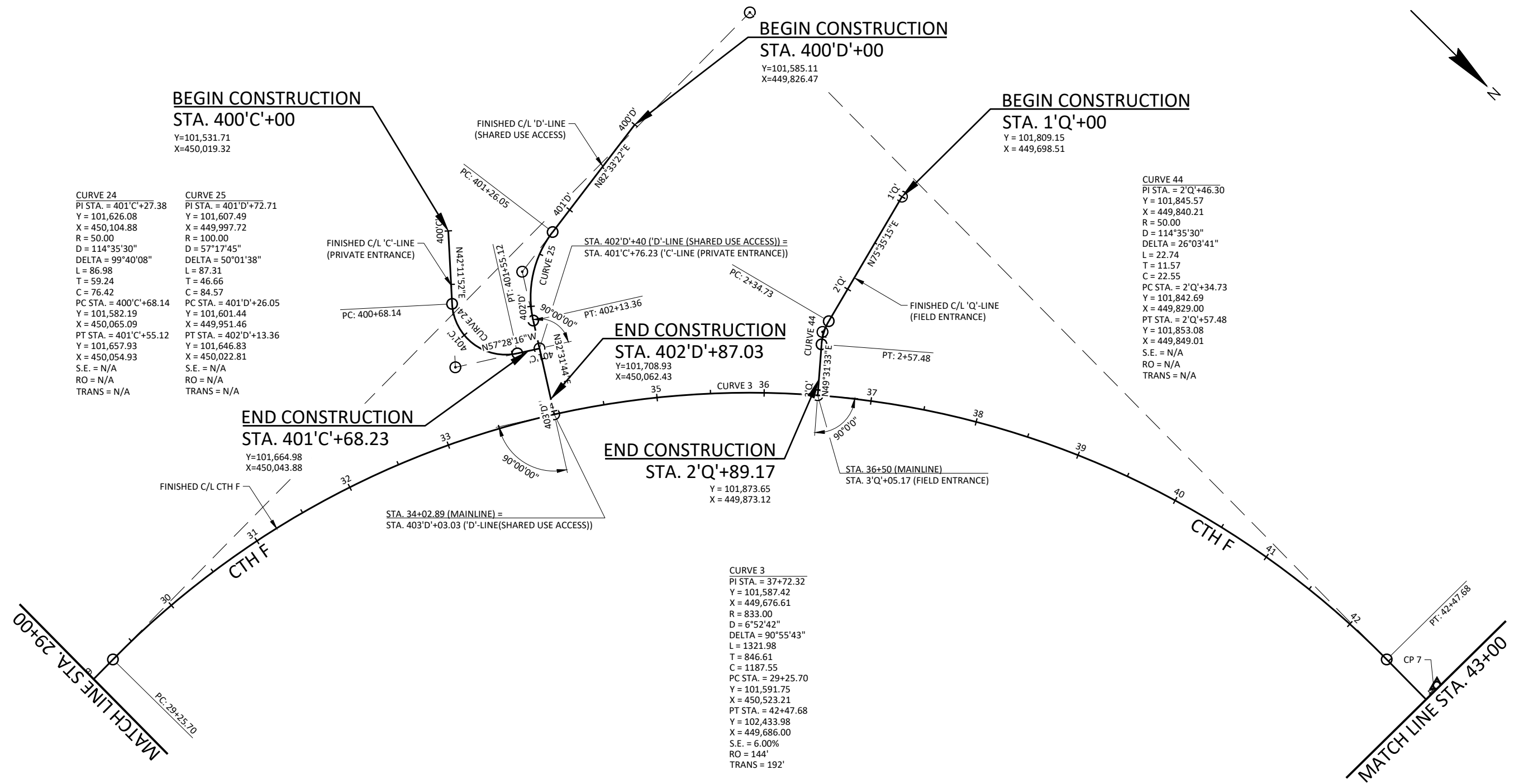
BEGIN CONSTRUCTION
STA. 1'Q'+00
 Y = 101,809.15
 X = 449,698.51

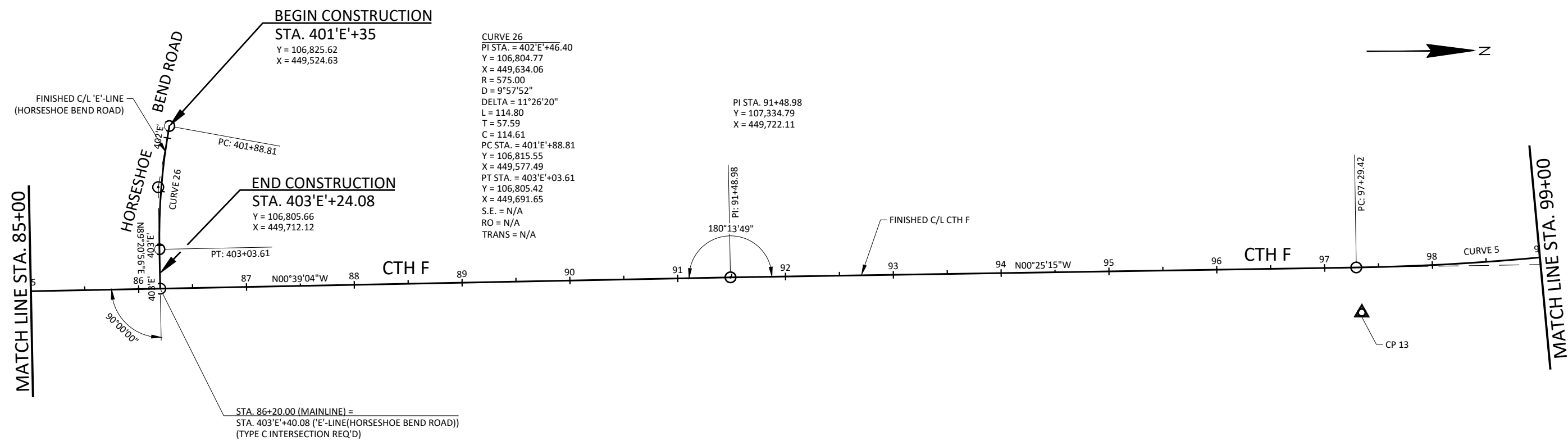
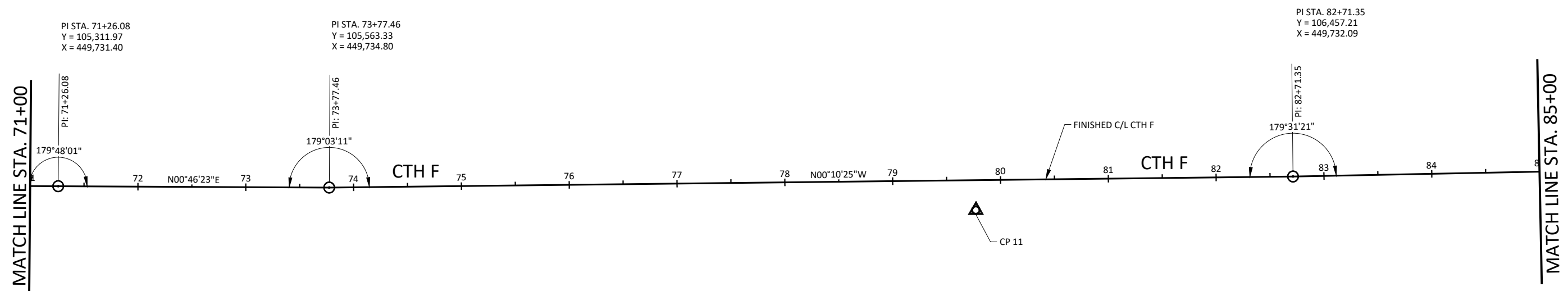
CURVE 24
 PI STA. = 401'C'+27.38
 Y = 101,626.08
 X = 450,104.88
 R = 50.00
 D = 114°35'30"
 DELTA = 99°40'08"
 L = 86.98
 T = 59.24
 C = 76.42
 PC STA. = 400'C'+68.14
 Y = 101,582.19
 X = 450,065.09
 PT STA. = 401'C'+55.12
 Y = 101,657.93
 X = 450,054.93
 S.E. = N/A
 RO = N/A
 TRANS = N/A

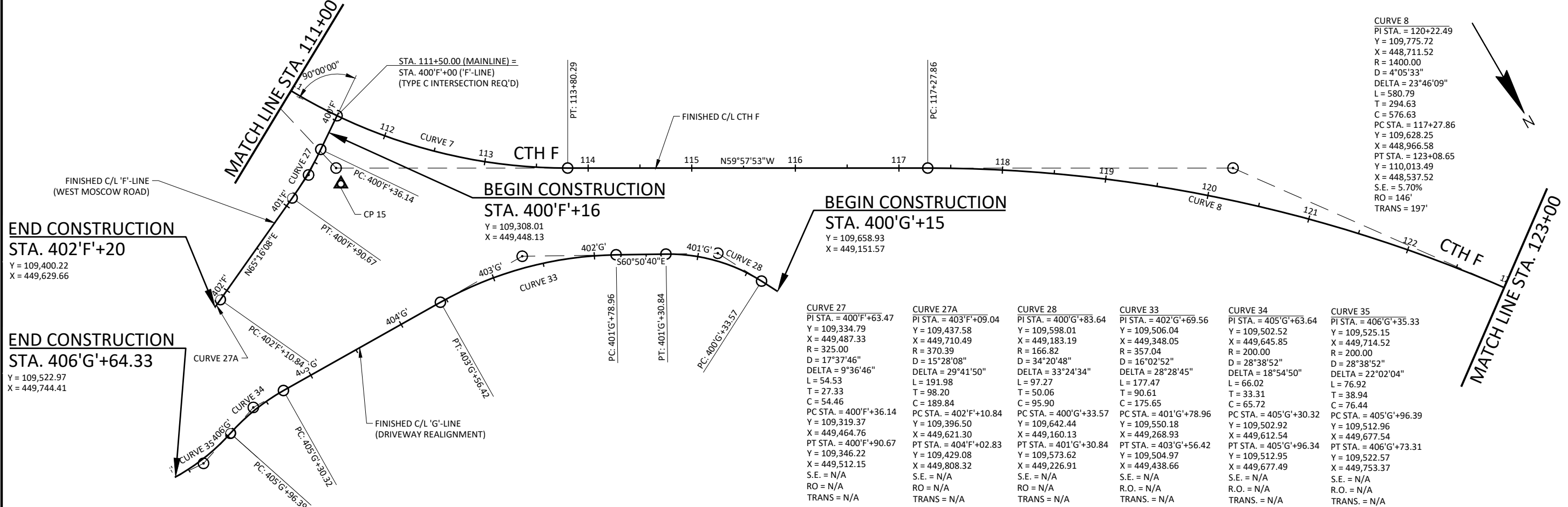
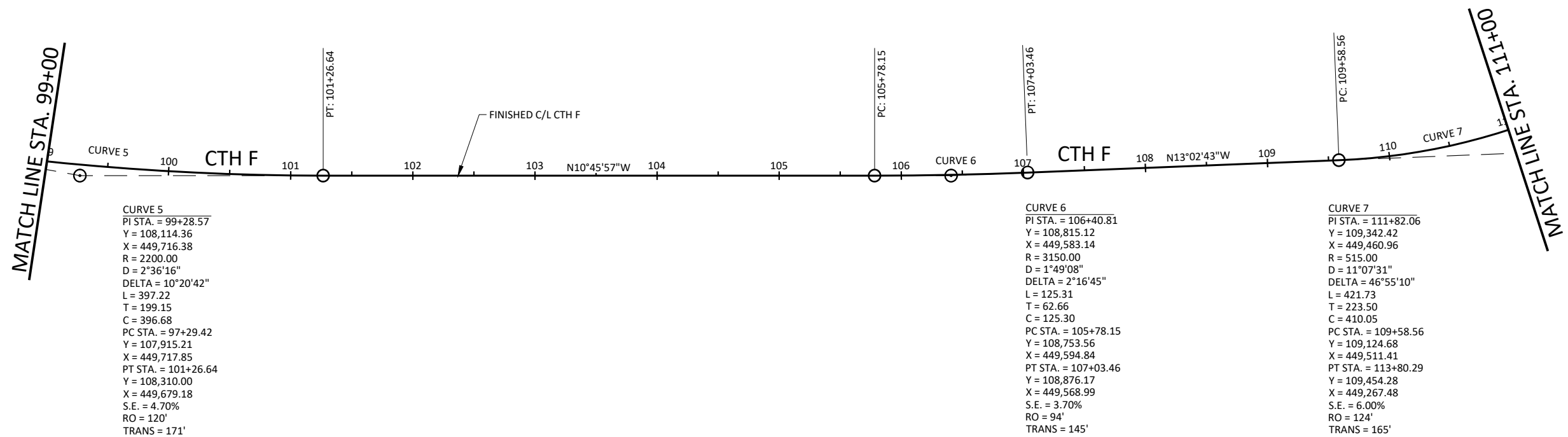
CURVE 25
 PI STA. = 401'D'+72.71
 Y = 101,607.49
 X = 449,997.72
 R = 100.00
 D = 57°17'45"
 DELTA = 50°01'38"
 L = 87.31
 T = 46.66
 C = 84.57
 PC STA. = 401'D'+26.05
 Y = 101,601.44
 X = 449,951.46
 PT STA. = 402'D'+13.36
 Y = 101,646.83
 X = 450,022.81
 S.E. = N/A
 RO = N/A
 TRANS = N/A

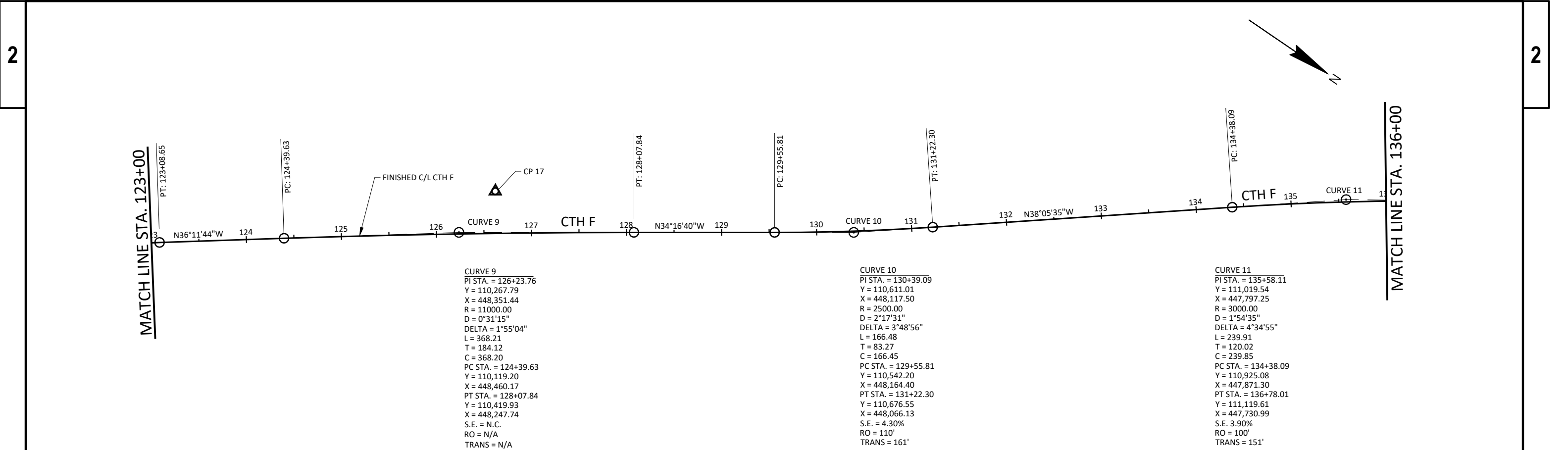
CURVE 44
 PI STA. = 2'Q'+46.30
 Y = 101,845.57
 X = 449,840.21
 R = 50.00
 D = 114°35'30"
 DELTA = 26°03'41"
 L = 22.74
 T = 11.57
 C = 22.55
 PC STA. = 2'Q'+34.73
 Y = 101,842.69
 X = 449,829.00
 PT STA. = 2'Q'+57.48
 Y = 101,853.08
 X = 449,849.01
 S.E. = N/A
 RO = N/A
 TRANS = N/A

CURVE 3
 PI STA. = 37+72.32
 Y = 101,587.42
 X = 449,676.61
 R = 833.00
 D = 6°52'42"
 DELTA = 90°55'43"
 L = 1321.98
 T = 846.61
 C = 1187.55
 PC STA. = 29+25.70
 Y = 101,591.75
 X = 450,523.21
 PT STA. = 42+47.68
 Y = 102,433.98
 X = 449,686.00
 S.E. = 6.00%
 RO = 144'
 TRANS = 192'









CURVE 9
 PI STA. = 126+23.76
 Y = 110,267.79
 X = 448,351.44
 R = 11000.00
 D = 0°31'15"
 DELTA = 1°55'04"
 L = 368.21
 T = 184.12
 C = 368.20
 PC STA. = 124+39.63
 Y = 110,119.20
 X = 448,460.17
 PT STA. = 128+07.84
 Y = 110,419.93
 X = 448,247.74
 S.E. = N.C.
 RO = N/A
 TRANS = N/A

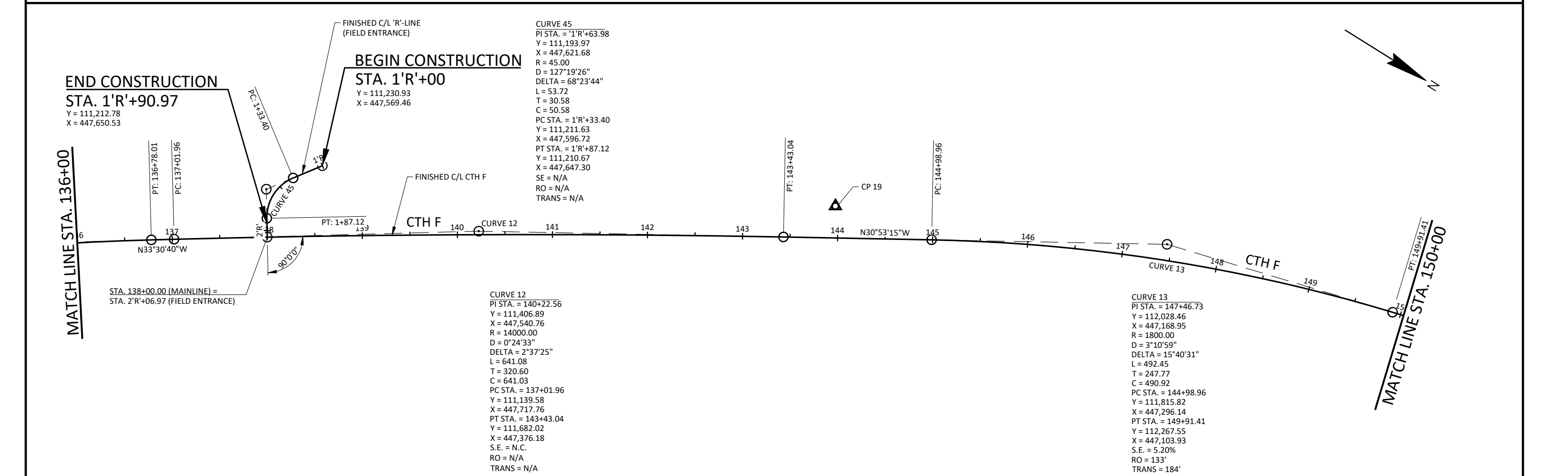
CURVE 10
 PI STA. = 130+39.09
 Y = 110,611.01
 X = 448,117.50
 R = 2500.00
 D = 2°17'31"
 DELTA = 3°48'56"
 L = 166.48
 T = 83.27
 C = 166.45
 PC STA. = 129+55.81
 Y = 110,542.20
 X = 448,164.40
 PT STA. = 131+22.30
 Y = 110,676.55
 X = 448,066.13
 S.E. = 4.30%
 RO = 110'
 TRANS = 161'

CURVE 11
 PI STA. = 135+58.11
 Y = 111,019.54
 X = 447,797.25
 R = 3000.00
 D = 1°54'35"
 DELTA = 4°34'55"
 L = 239.91
 T = 120.02
 C = 239.85
 PC STA. = 134+38.09
 Y = 110,925.08
 X = 447,871.30
 PT STA. = 136+78.01
 Y = 111,119.61
 X = 447,730.99
 S.E. = 3.90%
 RO = 100'
 TRANS = 151'

CURVE 45
 PI STA. = 1'R'+63.98
 Y = 111,193.97
 X = 447,621.68
 R = 45.00
 D = 127°19'26"
 DELTA = 68°23'44"
 L = 53.72
 T = 30.58
 C = 50.58
 PC STA. = 1'R'+33.40
 Y = 111,211.63
 X = 447,596.72
 PT STA. = 1'R'+87.12
 Y = 111,210.67
 X = 447,647.30
 SE = N/A
 RO = N/A
 TRANS = N/A

CURVE 12
 PI STA. = 140+22.56
 Y = 111,406.89
 X = 447,540.76
 R = 14000.00
 D = 0°24'33"
 DELTA = 2°37'25"
 L = 641.08
 T = 320.60
 C = 641.03
 PC STA. = 137+01.96
 Y = 111,139.58
 X = 447,717.76
 PT STA. = 143+43.04
 Y = 111,682.02
 X = 447,376.18
 S.E. = N.C.
 RO = N/A
 TRANS = N/A

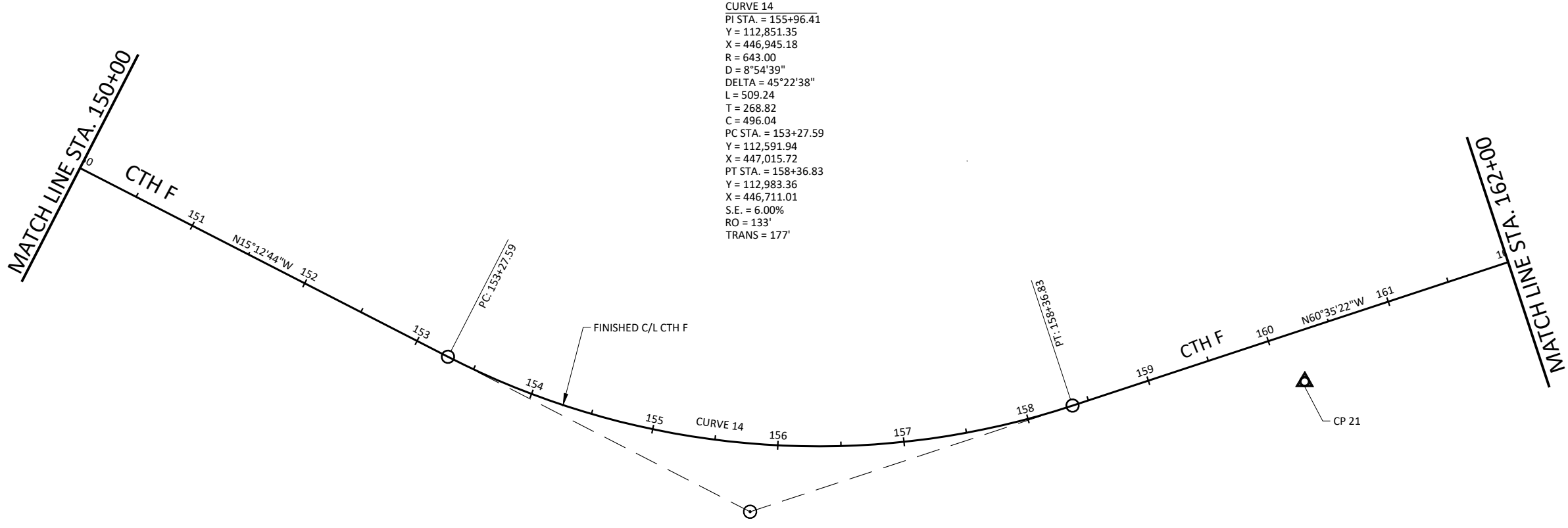
CURVE 13
 PI STA. = 147+46.73
 Y = 112,028.46
 X = 447,168.95
 R = 1800.00
 D = 3°10'59"
 DELTA = 15°40'31"
 L = 492.45
 T = 247.77
 C = 490.92
 PC STA. = 144+98.96
 Y = 111,815.82
 X = 447,296.14
 PT STA. = 149+91.41
 Y = 112,267.55
 X = 447,103.93
 S.E. = 5.20%
 RO = 133'
 TRANS = 184'



END CONSTRUCTION
 STA. 1'R'+90.97
 Y = 111,212.78
 X = 447,650.53

BEGIN CONSTRUCTION
 STA. 1'R'+00
 Y = 111,230.93
 X = 447,569.46

STA. 138+00.00 (MAINLINE) =
 STA. 2'R'+06.97 (FIELD ENTRANCE)



CURVE 14
 PI STA. = 155+96.41
 Y = 112,851.35
 X = 446,945.18
 R = 643.00
 D = 8°54'39"
 DELTA = 45°22'38"
 L = 509.24
 T = 268.82
 C = 496.04
 PC STA. = 153+27.59
 Y = 112,591.94
 X = 447,015.72
 PT STA. = 158+36.83
 Y = 112,983.36
 X = 446,711.01
 S.E. = 6.00%
 RO = 133'
 TRANS = 177'

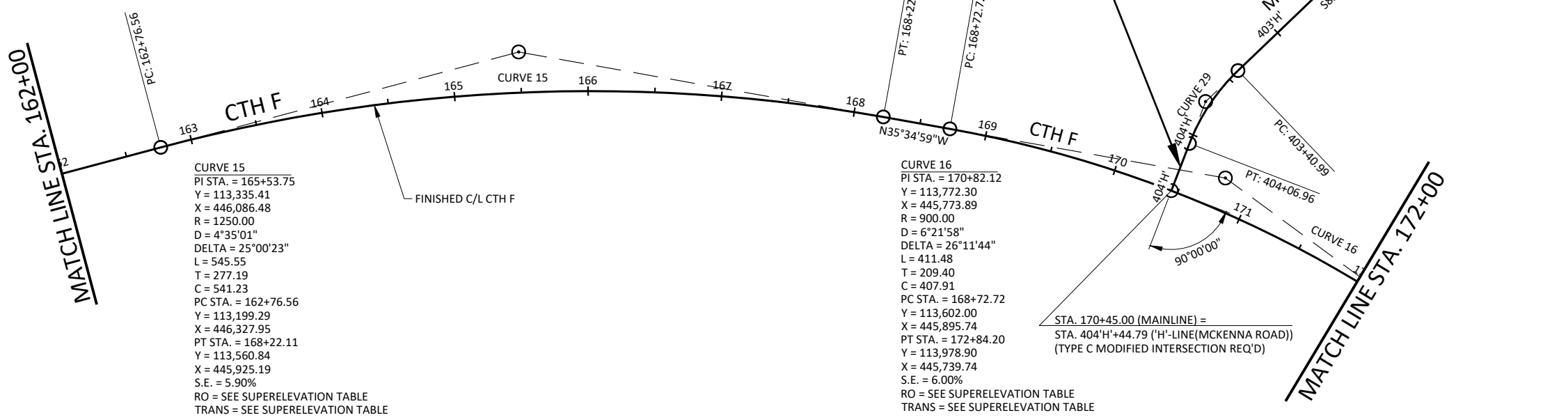
MATCH LINE STA. 162+00

CURVE 15
 PI STA. = 165+53.75
 Y = 113,335.41
 X = 446,086.48
 R = 1250.00
 D = 4°35'01"
 DELTA = 25°00'23"
 L = 545.55
 T = 277.19
 C = 541.23
 PC STA. = 162+76.56
 Y = 113,199.29
 X = 446,327.95
 PT STA. = 168+22.11
 Y = 113,560.84
 X = 445,925.19
 S.E. = 5.90%
 RO = SEE SUPERELEVATION TABLE
 TRANS = SEE SUPERELEVATION TABLE

END CONSTRUCTION
STA. 404'H'+28.79
 Y = 113,744.16
 X = 445,794.93

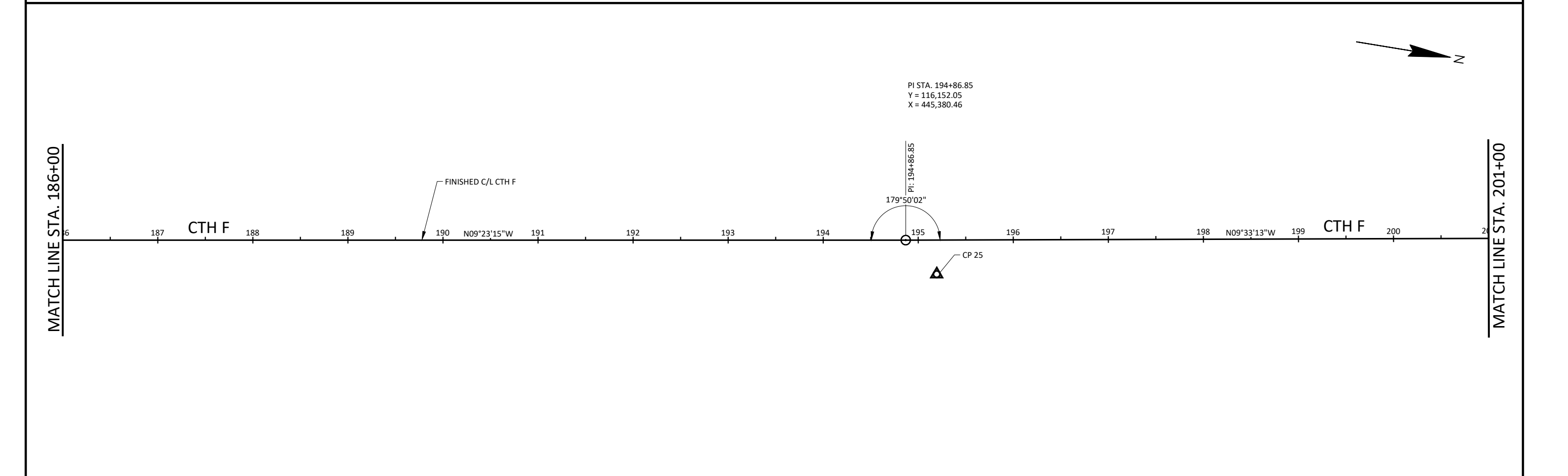
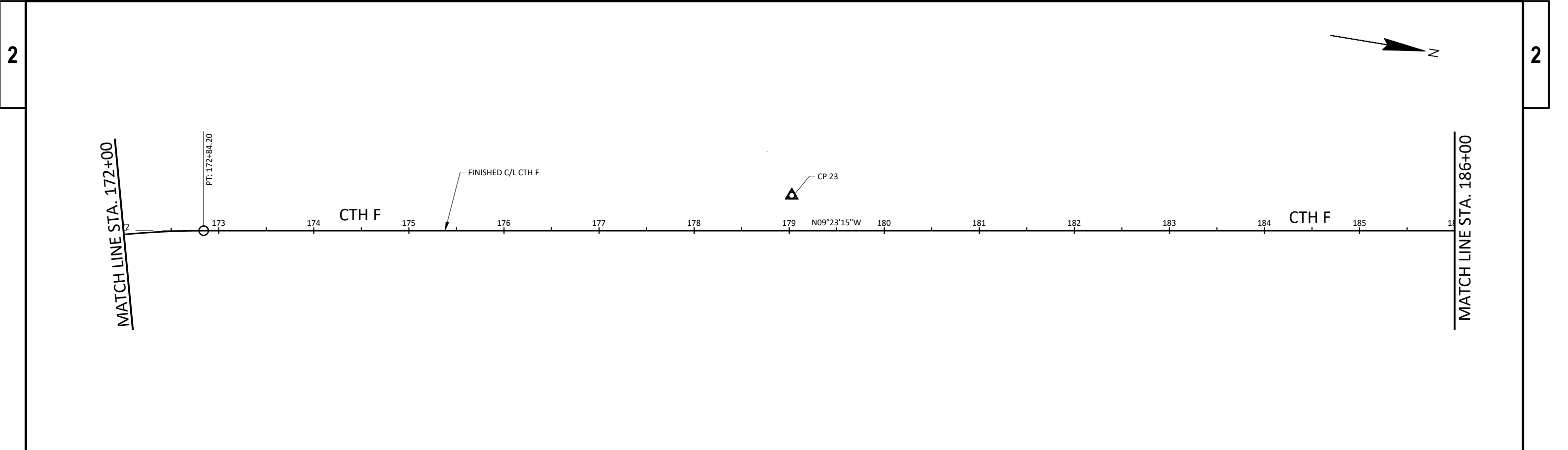
BEGIN CONSTRUCTION
STA. 400'H'+20
 Y = 113,724.73
 X = 445,390.10

CURVE 29
 PI STA. = 403'H'+74.52
 Y = 113,721.10
 X = 445,744.60
 R = 150.00
 D = 38°11'50"
 DELTA = 25°12'08"
 L = 65.98
 T = 33.53
 C = 65.45
 PC STA. = 403'H'+40.99
 Y = 113,721.44
 X = 445,711.07
 PT STA. = 404'H'+06.96
 Y = 113,735.06
 X = 445,775.09
 S.E. = N/A
 RO = N/A
 TRANS = N/A



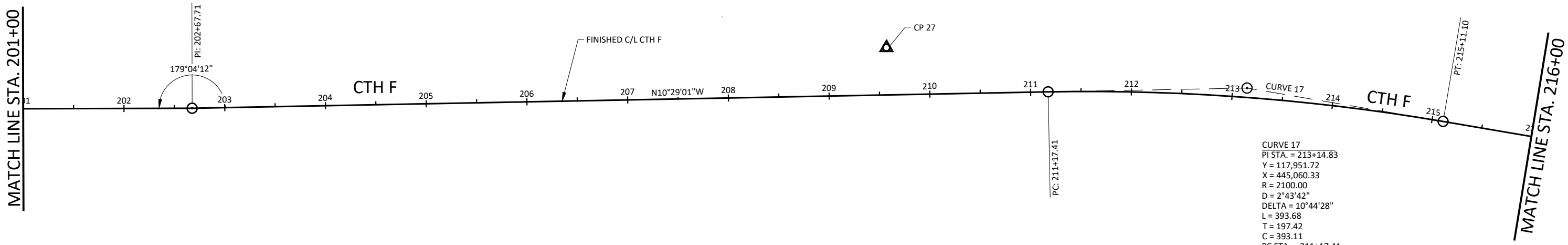
CURVE 16
 PI STA. = 170+82.12
 Y = 113,772.30
 X = 445,773.89
 R = 900.00
 D = 6°21'58"
 DELTA = 26°11'44"
 L = 411.48
 T = 209.40
 C = 407.91
 PC STA. = 168+72.72
 Y = 113,602.00
 X = 445,895.74
 PT STA. = 172+84.20
 Y = 113,978.90
 X = 445,739.74
 S.E. = 6.00%
 RO = SEE SUPERELEVATION TABLE
 TRANS = SEE SUPERELEVATION TABLE

STA. 170+45.00 (MAINLINE) =
 STA. 404'H'+44.79 ('H'-LINE(MCKENNA ROAD))
 (TYPE C MODIFIED INTERSECTION REQ'D)



PROJECT NO: 5034-00-72	HWY: CTH F	COUNTY: IOWA	ALIGNMENT DETAILS	SHEET	E
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PI STA. 202+67.71
Y = 116,922.07
X = 445,250.86

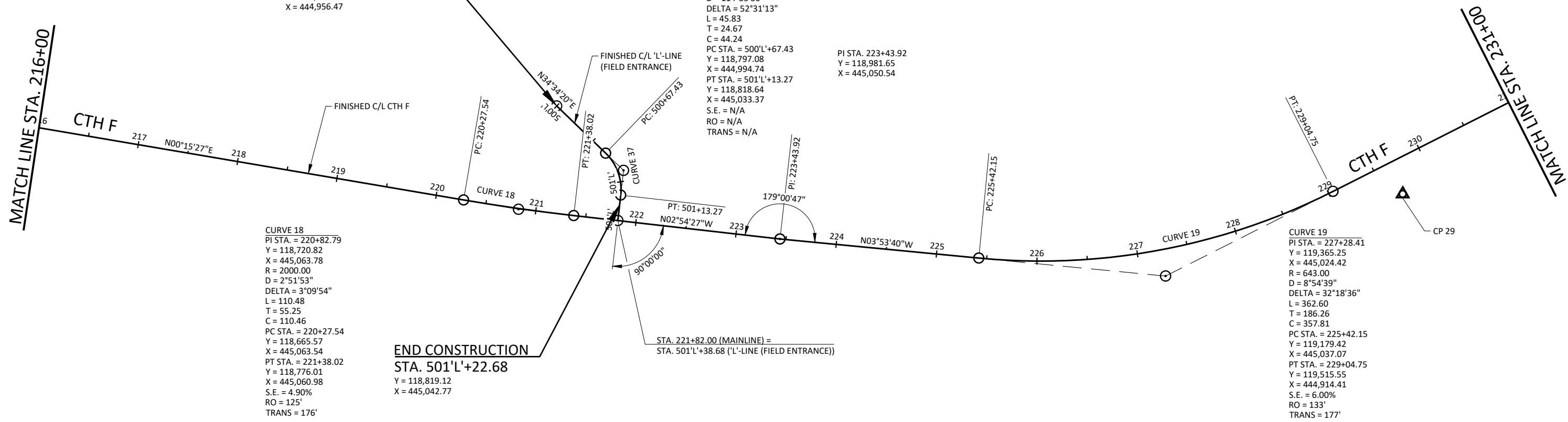


CURVE 17
 PI STA. = 213+14.83
 Y = 117,951.72
 X = 445,060.33
 R = 2100.00
 D = 2°43'42"
 DELTA = 10°44'28"
 L = 393.68
 T = 197.42
 C = 393.11
 PC STA. = 211+17.41
 Y = 117,757.59
 X = 445,096.25
 PT STA. = 215+11.10
 Y = 118,149.14
 X = 445,061.22
 S.E. = 4.80%
 RO = 123'
 TRANS = 174'

BEGIN CONSTRUCTION
 STA. 500'L'+00
 Y = 118,741.56
 X = 444,956.47

CURVE 37
 PI STA. = 500'L'+92.10
 Y = 118,817.39
 X = 445,008.74
 R = 50.00
 D = 114°35'30"
 DELTA = 52°31'13"
 L = 45.83
 T = 24.67
 C = 44.24
 PC STA. = 500'L'+67.43
 Y = 118,797.08
 X = 444,994.74
 PT STA. = 501'L'+13.27
 Y = 118,818.64
 X = 445,033.37
 S.E. = N/A
 RO = N/A
 TRANS = N/A

PI STA. 223+43.92
 Y = 118,981.65
 X = 445,050.54

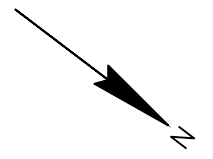


CURVE 18
 PI STA. = 220+82.79
 Y = 118,720.82
 X = 445,063.78
 R = 2000.00
 D = 2°51'53"
 DELTA = 3°09'54"
 L = 110.48
 T = 55.25
 C = 110.46
 PC STA. = 220+27.54
 Y = 118,665.57
 X = 445,063.54
 PT STA. = 221+38.02
 Y = 118,776.01
 X = 445,060.98
 S.E. = 4.90%
 RO = 125'
 TRANS = 176'

END CONSTRUCTION
 STA. 501'L'+22.68
 Y = 118,819.12
 X = 445,042.77

CURVE 19
 PI STA. = 227+28.41
 Y = 119,365.25
 X = 445,024.42
 R = 643.00
 D = 8°54'39"
 DELTA = 32°18'36"
 L = 362.60
 T = 186.26
 C = 357.81
 PC STA. = 225+42.15
 Y = 119,179.42
 X = 445,037.07
 PT STA. = 229+04.75
 Y = 119,515.55
 X = 444,914.41
 S.E. = 6.00%
 RO = 133'
 TRANS = 177'

PROJECT NO: 5034-00-72	HWY: CTH F	COUNTY: IOWA	ALIGNMENT DETAILS	SHEET	E
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BEGIN CONSTRUCTION
 STA. 401'I'+70
 Y = 120,185.98
 X = 443,599.38

CURVE 31
 PI STA. = 402'I'+26.81
 Y = 120,190.48
 X = 443,656.01
 R = 2000.00
 D = 2°51'53"
 DELTA = 4°30'21"
 L = 157.28
 T = 78.68
 C = 157.24
 PC STA. = 401'I'+48.13
 Y = 120,184.42
 X = 443,577.56
 PT STA. = 403'I'+05.41
 Y = 120,190.36
 X = 443,734.69
 S.E. = N/A
 RO = N/A
 TRANS = N/A

CURVE 38
 PI STA. = 501'M'+05.05
 Y = 120,109.11
 X = 444,423.15
 R = 50.00
 D = 114°35'30"
 DELTA = 51°50'03"
 L = 45.23
 T = 24.30
 C = 43.71
 PC STA. = 500'M'+80.75
 Y = 120,084.81
 X = 444,422.97
 PT STA. = 501'M'+25.99
 Y = 120,123.98
 X = 444,442.37
 S.E. = N/A
 RO = N/A
 TRANS = N/A

CURVE 20
 PI STA. = 237+57.49
 Y = 120,199.72
 X = 444,405.46
 R = 5000.00
 D = 1°08'45"
 DELTA = 2°59'13"
 L = 260.66
 T = 130.36
 C = 260.63
 PC STA. = 236+27.13
 Y = 120,095.86
 X = 444,484.25
 PT STA. = 238+87.79
 Y = 120,299.32
 X = 444,321.37
 S.E. = 2.70%
 RO = 69'
 TRANS = 120'

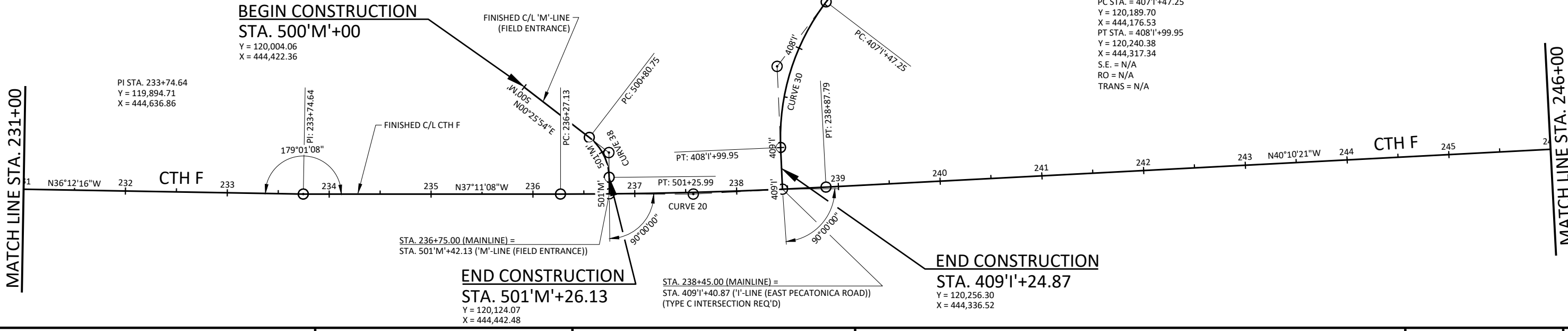
CURVE 30
 PI STA. = 408'I'+26.82
 Y = 120,189.58
 X = 444,256.10
 R = 220.00
 D = 26°02'37"
 DELTA = 39°46'07"
 L = 152.70
 T = 79.57
 C = 149.65
 PC STA. = 407'I'+47.25
 Y = 120,189.70
 X = 444,176.53
 PT STA. = 408'I'+99.95
 Y = 120,240.38
 X = 444,317.34
 S.E. = N/A
 RO = N/A
 TRANS = N/A

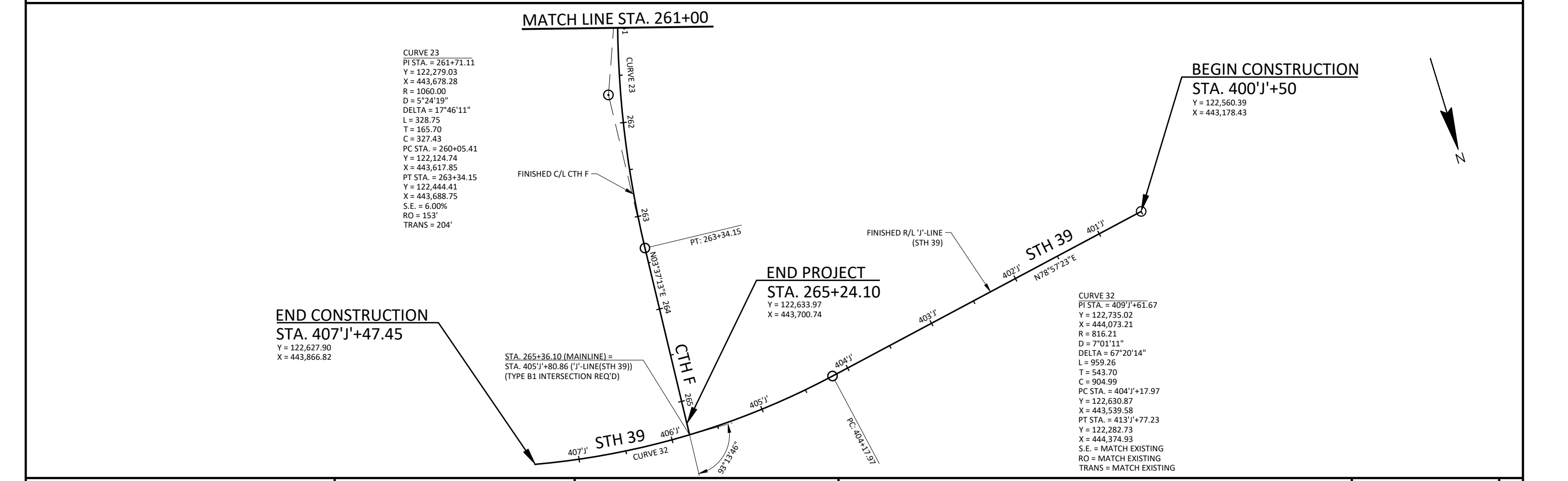
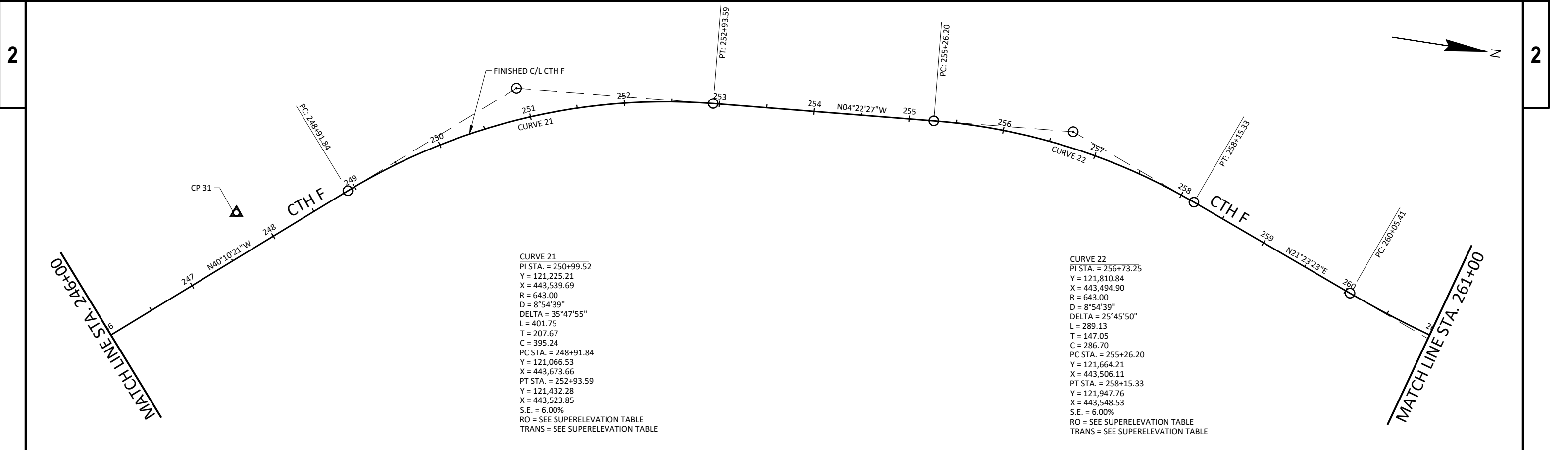
BEGIN CONSTRUCTION
 STA. 500'M'+00
 Y = 120,004.06
 X = 444,422.36

PI STA. 233+74.64
 Y = 119,894.71
 X = 444,636.86

MATCH LINE STA. 231+00

MATCH LINE STA. 246+00





PROJECT NO: 5034-00-72	HWY: CTH F	COUNTY: IOWA	ALIGNMENT DETAILS	SHEET	E
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MAINLINE (CTH F) STATION LAYOUT

Table with 4 columns: Station, X, Y, Remarks. Contains stationing data from 10+24.00 to 75+00.00.

MAINLINE (CTH F) STATION LAYOUT

Table with 4 columns: Station, X, Y, Remarks. Contains stationing data from 76+00.00 to 141+00.00.

MAINLINE (CTH F) STATION LAYOUT

Table with 4 columns: Station, X, Y, Remarks. Contains stationing data from 142+00.00 to 207+00.00.

MAINLINE (CTH F) STATION LAYOUT

Table with 4 columns: Station, X, Y, Remarks. Contains stationing data from 208+00.00 to 265+24.10.

'A'-LINE (STH 78) STATION LAYOUT

Station	Y	X	Remarks
399'A'+55.00	101,547.95	452,411.16	Begin Construction
400'A'+00.00	101,592.86	452,408.23	-
400'A'+50.00	101,642.77	452,405.26	-
401'A'+00.00	101,692.70	452,402.59	-
401'A'+50.00	101,742.64	452,400.22	-
402'A'+00.00	101,792.61	452,398.55	-
402'A'+50.00	101,842.59	452,396.88	-
403'A'+00.00	101,892.56	452,395.21	-
403'A'+50.00	101,942.53	452,393.59	-
404'A'+00.00	101,992.51	452,392.04	-
404'A'+50.00	102,042.48	452,390.48	-
405'A'+00.00	102,092.46	452,388.92	-
405'A'+50.00	102,142.44	452,387.42	-
406'A'+00.00	102,192.41	452,385.89	-
406'A'+50.00	102,242.38	452,384.15	-
406'A'+98.30	102,290.65	452,382.47	End Construction

'E'-LINE (HORSESHOE BEND ROAD) STATION LAYOUT

Station	Y	X	Remarks
401'E'+35.00	106,825.62	449,524.63	Begin Construction
401'E'+50.00	106,822.81	449,539.36	-
402'E'+00.00	106,813.56	449,588.50	-
402'E'+50.00	106,807.31	449,638.09	-
403'E'+00.00	106,805.40	449,688.04	-
403'E'+24.08	106,805.66	449,712.12	End Construction

'I'-LINE (EAST PECATONICA ROAD) STATION LAYOUT

Station	Y	X	Remarks
401'I'+70.00	120,185.98	443,599.38	Begin Construction
402'I'+00.00	120,187.74	443,629.33	-
402'I'+50.00	120,189.68	443,679.29	-
403'I'+00.00	120,190.36	443,729.28	-
403'I'+50.00	120,190.29	443,779.28	-
404'I'+00.00	120,190.22	443,829.28	-
404'I'+50.00	120,190.14	443,879.28	-
405'I'+00.00	120,190.07	443,929.28	-
405'I'+50.00	120,189.99	443,979.28	-
406'I'+00.00	120,189.92	444,029.28	-
406'I'+50.00	120,189.84	444,079.28	-
407'I'+00.00	120,189.77	444,129.28	-
407'I'+50.00	120,189.71	444,179.28	-
408'I'+00.00	120,195.91	444,228.79	-
408'I'+50.00	120,213.11	444,275.62	-
409'I'+00.00	120,240.42	444,317.38	-
409'I'+24.87	120,256.30	444,336.52	End Construction

'F'-LINE (WEST MOSCOW ROAD) STATION LAYOUT

Station	Y	X	Remarks
400'F'+16.00	109,308.01	449,448.13	Begin Construction
400'F'+50.00	109,326.94	449,476.37	-
401'F'+00.00	109,350.12	449,520.62	-
401'F'+50.00	109,371.04	449,566.04	-
402'F'+00.00	109,391.96	449,611.45	-
402'F'+20.00	109,400.22	449,629.66	End Construction

'B'-LINE (MOUND STREET) STATION LAYOUT

Station	Y	X	Remarks
399'B'+62.87	101,337.53	450,995.50	Begin Construction
400'B'+00.00	101,374.65	450,995.03	-
400'B'+50.00	101,424.65	450,994.40	-
401'B'+00.00	101,474.64	450,993.78	-
401'B'+50.00	101,524.64	450,993.15	-
402'B'+00.00	101,574.64	450,992.52	-
402'B'+03.52	101,578.16	450,992.48	End Construction

'G'-LINE (DRIVEWAY) STATION LAYOUT

Station	Y	X	Remarks
400'G'+15.00	109,658.93	449,151.57	Begin Construction
400'G'+50.00	109,628.26	449,168.40	-
401'G'+00.00	109,591.04	449,201.51	-
401'G'+50.00	109,564.29	449,243.64	-
402'G'+00.00	109,540.48	449,287.60	-
402'G'+50.00	109,521.95	449,333.99	-
403'G'+00.00	109,510.08	449,382.52	-
403'G'+50.00	109,505.10	449,432.23	-
404'G'+00.00	109,504.45	449,482.23	-
404'G'+50.00	109,503.86	449,532.23	-
405'G'+00.00	109,503.27	449,582.22	-
405'G'+50.00	109,503.65	449,632.20	-
406'G'+00.00	109,514.06	449,680.98	-
406'G'+50.00	109,522.76	449,730.08	-
406'G'+64.33	109,522.97	449,744.41	End Construction

'J'-LINE (STH 39) STATION LAYOUT

Station	Y	X	Remarks
400'J'+50.00	122,560.39	443,178.43	Begin Construction
401'J'+00.00	122,569.96	443,227.50	-
401'J'+50.00	122,579.54	443,276.57	-
402'J'+00.00	122,589.12	443,325.65	-
402'J'+50.00	122,598.70	443,374.72	-
403'J'+00.00	122,608.28	443,423.80	-
403'J'+50.00	122,617.85	443,472.87	-
404'J'+00.00	122,627.43	443,521.94	-
404'J'+50.00	122,636.39	443,571.13	-
405'J'+00.00	122,642.52	443,620.75	-
405'J'+50.00	122,645.60	443,670.64	-
406'J'+00.00	122,645.61	443,720.64	-
406'J'+50.00	122,642.57	443,770.53	-
407'J'+00.00	122,636.48	443,820.15	-
407'J'+47.45	122,627.90	443,866.82	End Construction

'C'-LINE (DRIVEWAY) STATION LAYOUT

Station	Y	X	Remarks
400'C'+00.00	101,531.71	450,019.32	Begin Construction
400'C'+50.00	101,568.75	450,052.90	-
401'C'+00.00	101,610.82	450,077.80	-
401'C'+50.00	101,654.96	450,059.10	-
401'C'+68.23	101,664.98	450,043.88	End Construction

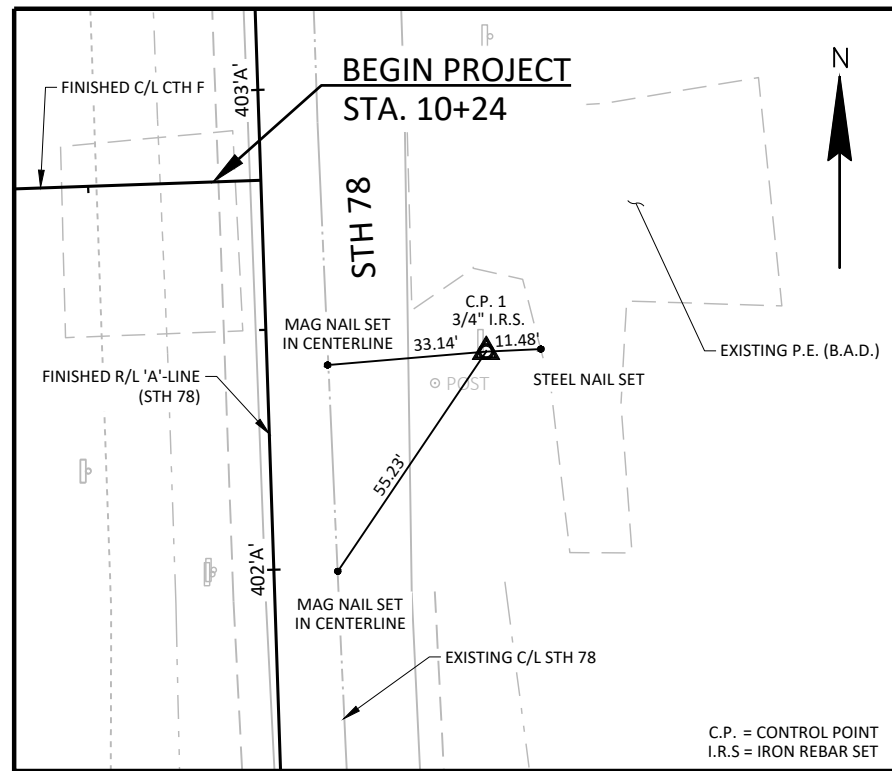
'H'-LINE (MCKENNA ROAD) STATION LAYOUT

Station	Y	X	Remarks
400'H'+20.00	113,724.73	445,390.10	Begin Construction
400'H'+50.00	113,724.42	445,420.10	-
401'H'+00.00	113,723.91	445,470.10	-
401'H'+50.00	113,723.40	445,520.10	-
402'H'+00.00	113,722.88	445,570.09	-
402'H'+50.00	113,722.37	445,620.09	-
403'H'+00.00	113,721.86	445,670.09	-
403'H'+50.00	113,721.62	445,720.08	-
404'H'+00.00	113,732.31	445,768.69	-
404'H'+28.79	113,744.16	445,794.93	End Construction

'D'-LINE (DRIVEWAY) STATION LAYOUT

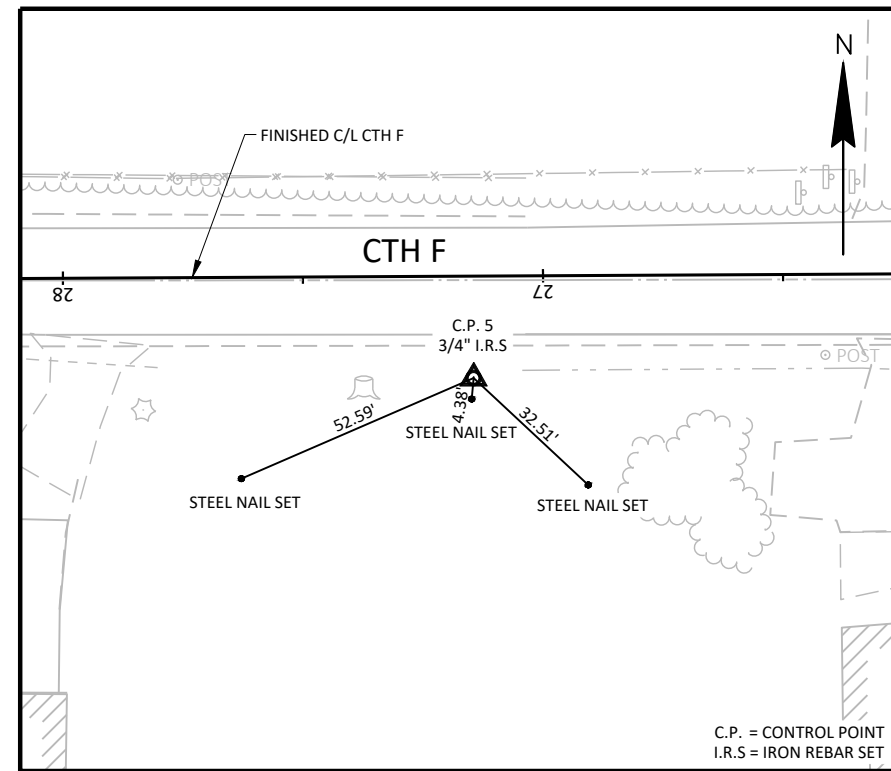
Station	Y	X	Remarks
400'D'+00.00	101,585.11	449,826.47	Begin Construction
400'D'+50.00	101,591.59	449,876.05	-
401'D'+00.00	101,598.07	449,925.63	-
401'D'+50.00	101,607.34	449,974.61	-
402'D'+00.00	101,636.07	450,014.90	-
402'D'+50.00	101,677.71	450,042.51	-
402'D'+87.03	101,708.93	450,062.43	End Construction

NOTE: REFER TO THE ALIGNMENT LAYOUT AS SHOWN ON THE PLAN & PROFILE SHEETS FOR THE FOLLOWING ALIGNMENTS: 'K'-LINE, 'L'-LINE, 'M'-LINE, 'N'-LINE, 'O'-LINE, 'P'-LINE, 'Q'-LINE, & 'R'-LINE



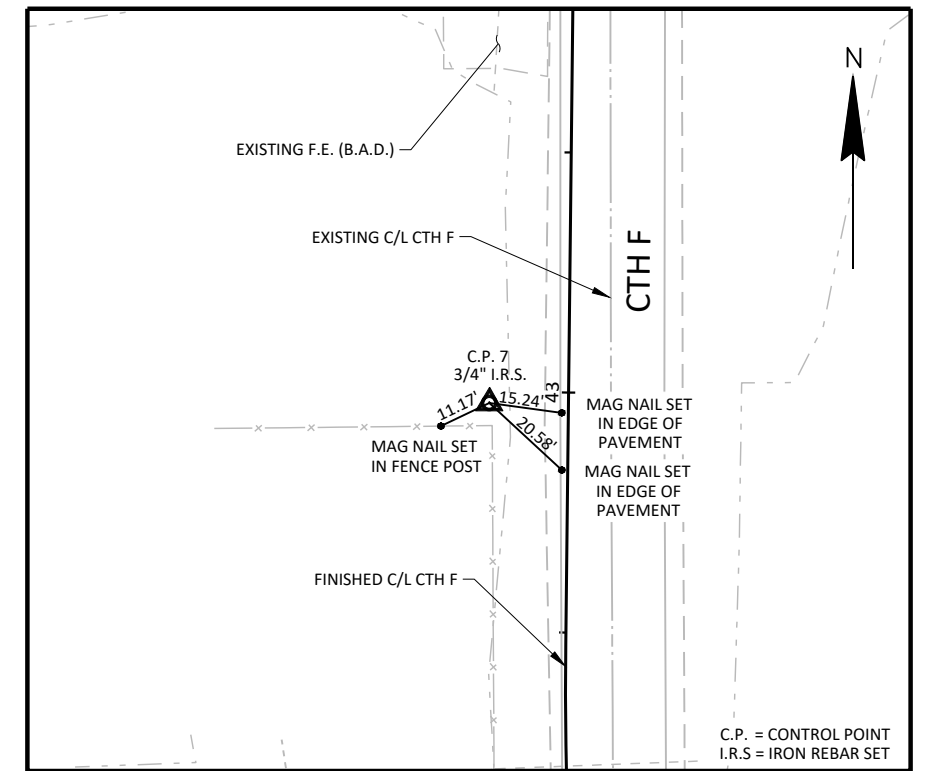
TIES TO C.P.#1

STA. 402'A+43.94, 45.71' RT
 Y = 101,838.06
 X = 452,442.77



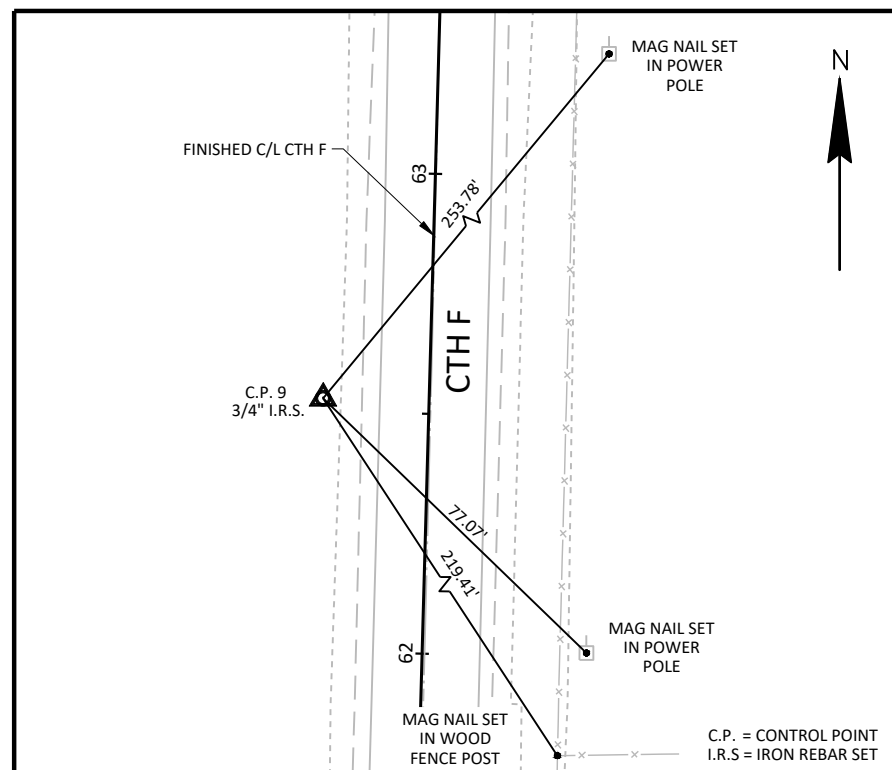
TIES TO C.P.#5

STA. 27+14.57, 21.23' LT
 Y = 101,571.60
 X = 450,734.45



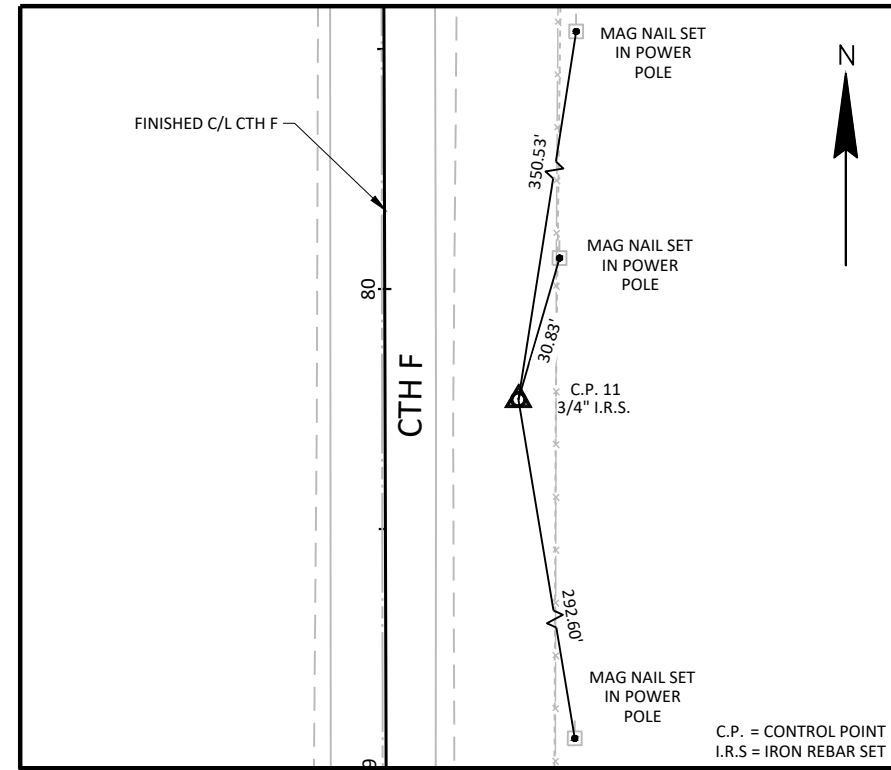
TIES TO C.P.#7

STA. 42+97.65, 16.46' LT
 Y = 102,484.13
 X = 449,670.10



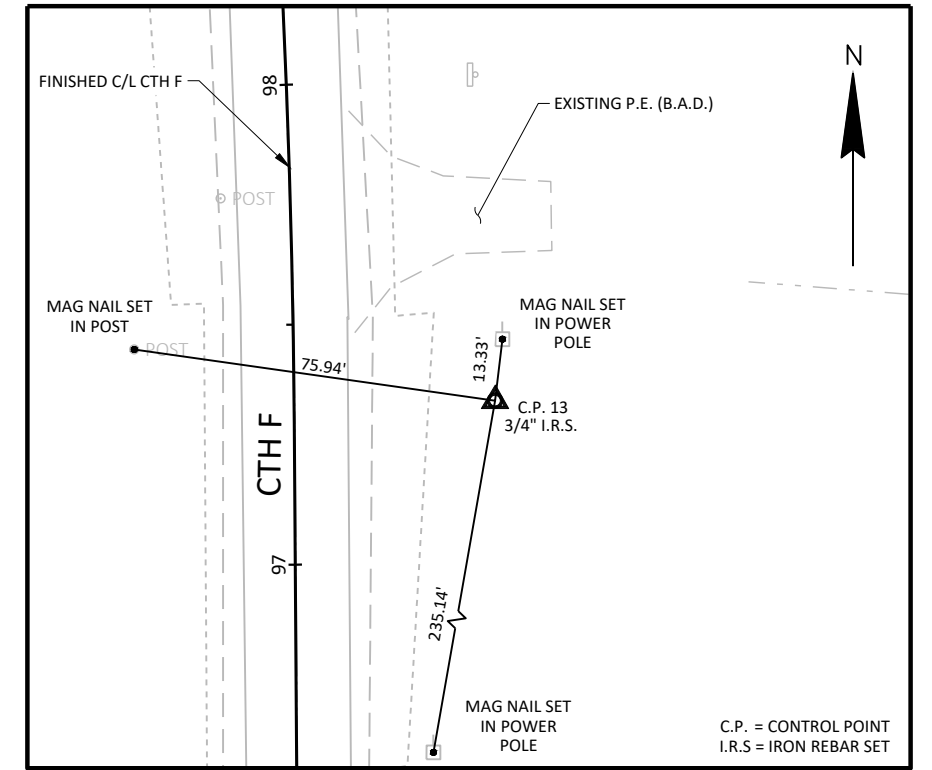
TIES TO C.P.#9

STA. 62+52.63, 22.10' LT
 Y = 104,439.29
 X = 449,693.07



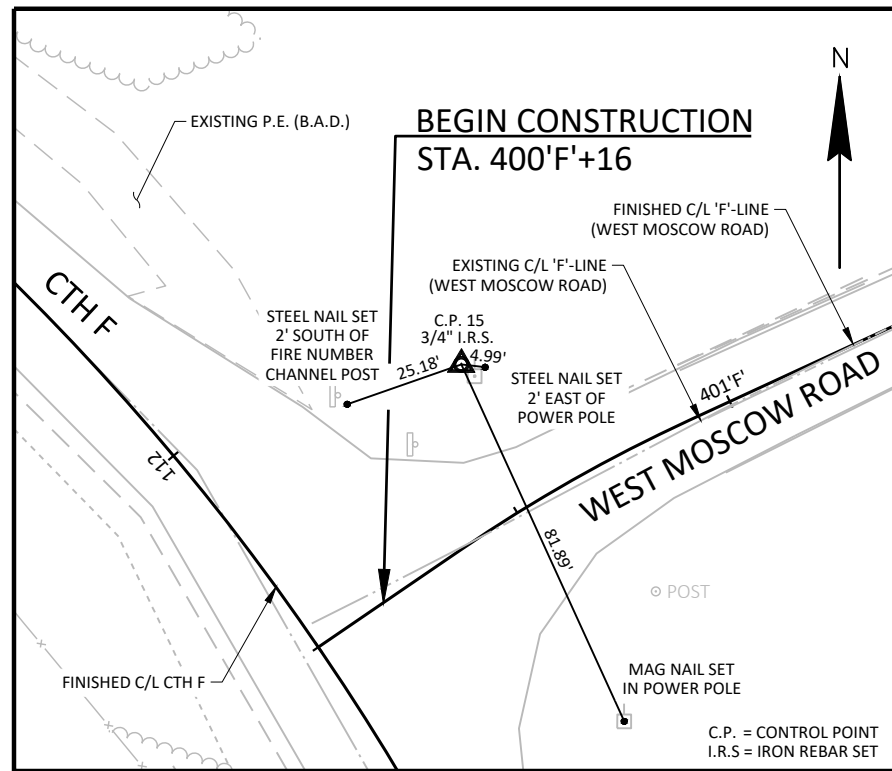
TIES TO C.P.#11

STA. 79+76.87, 27.79' RT
 Y = 106,162.81
 X = 449,760.77



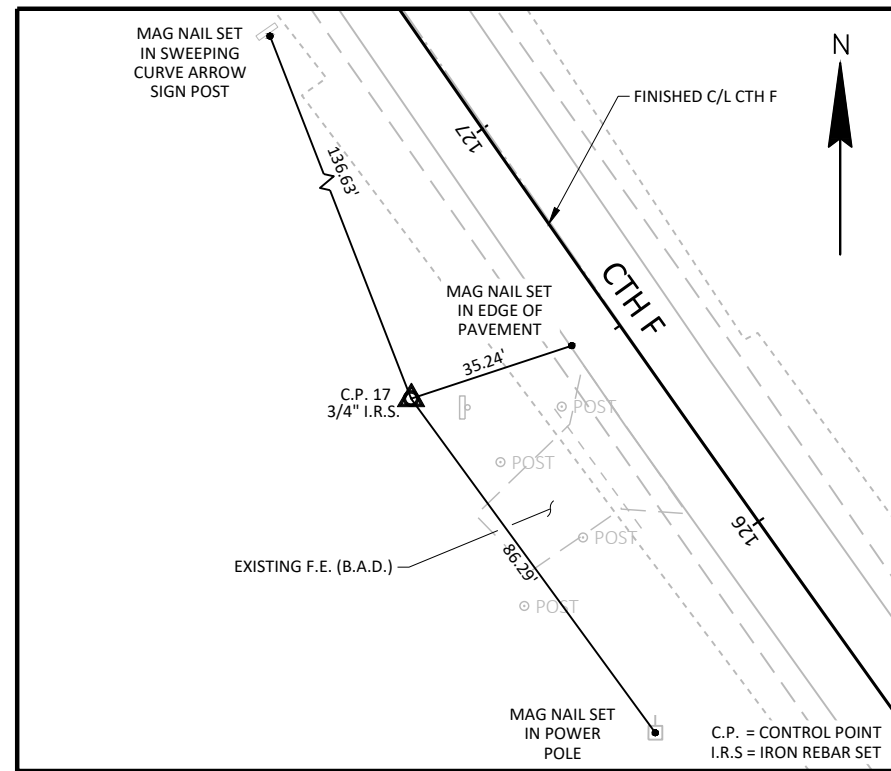
TIES TO C.P.#13

STA. 97+33.77, 41.90' RT
 Y = 107,919.95
 X = 449,759.71



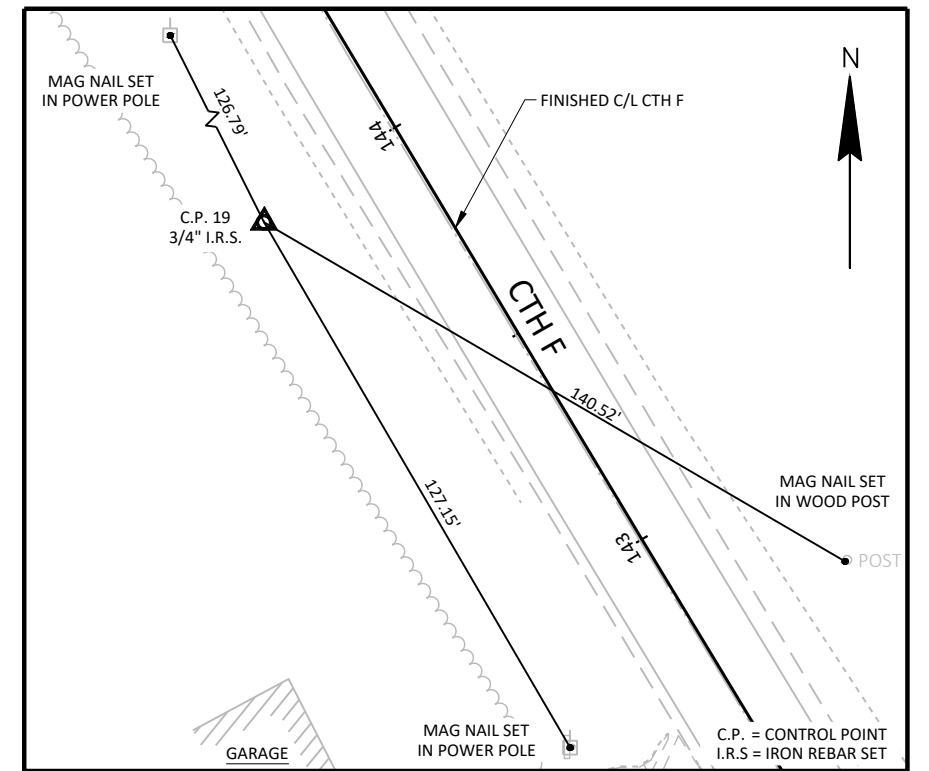
TIES TO C.P.#15

STA. 111+78.58, 58.69' RT
 Y = 109,357.87
 X = 449,464.70



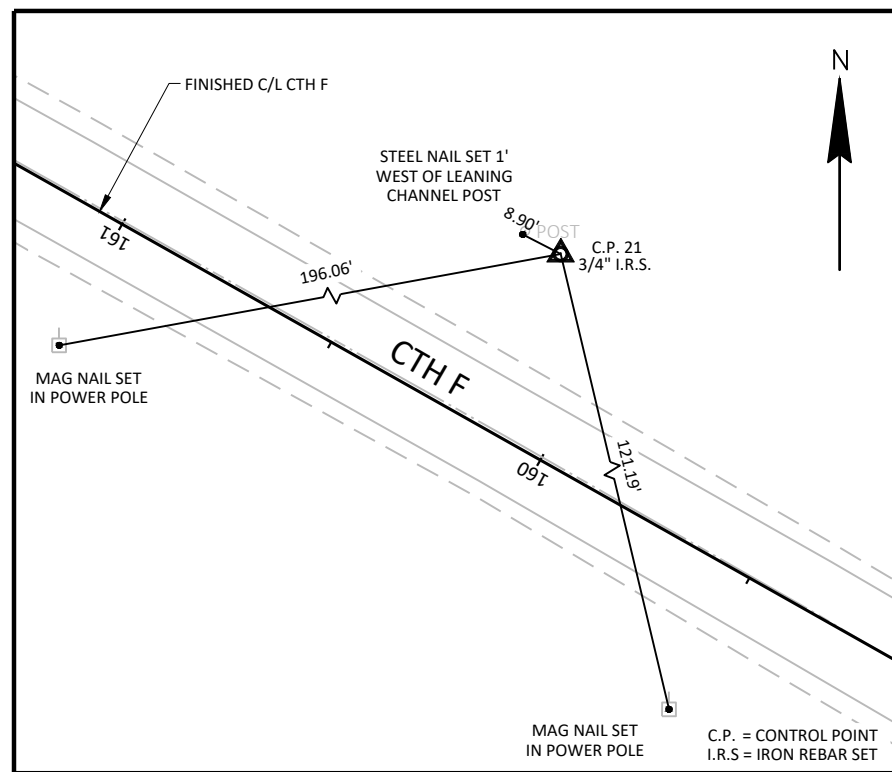
TIES TO C.P.#17

STA. 126+62.49, 44.32' LT
 Y = 110,274.92
 X = 448,294.10



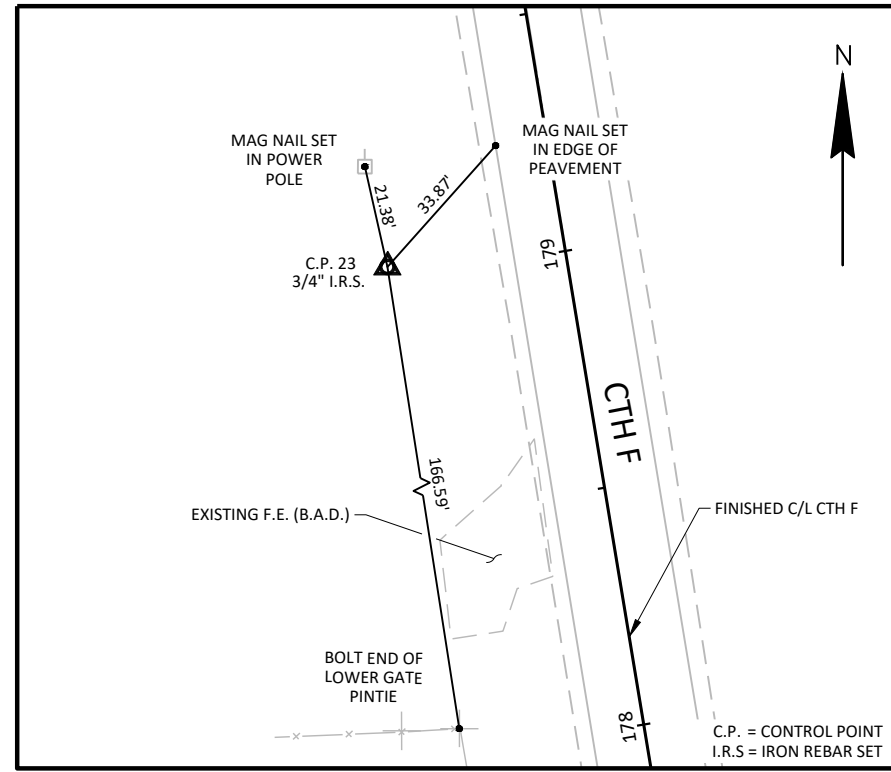
TIES TO C.P.#19

STA. 143+97.10, 33.56' LT
 Y = 111,711.18
 X = 447,319.63



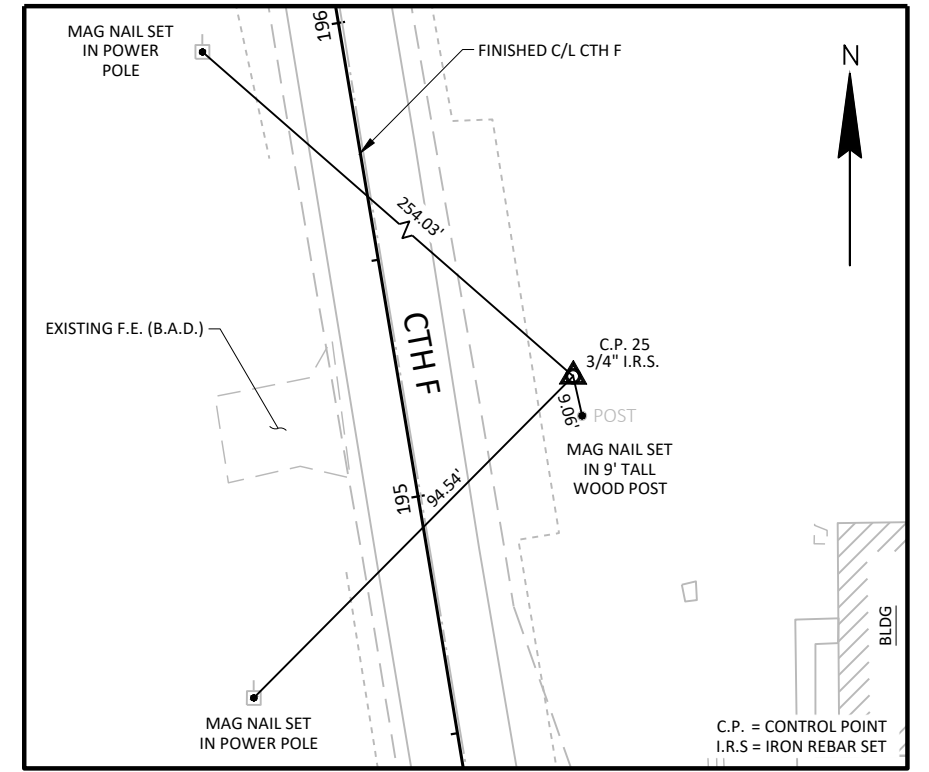
TIES TO C.P.#21

STA. 160+17.38, 39.46' RT
 Y = 113,106.39
 X = 446,573.11



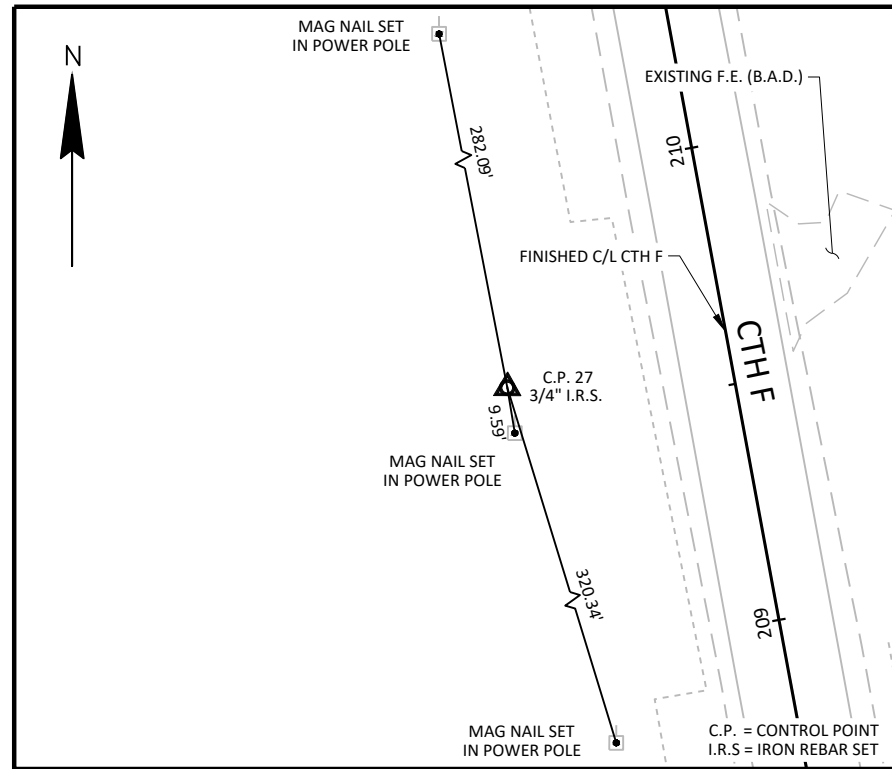
TIES TO C.P.#23

STA. 179+02.74, 37.10' LT
 Y = 114,583.09
 X = 445,602.25



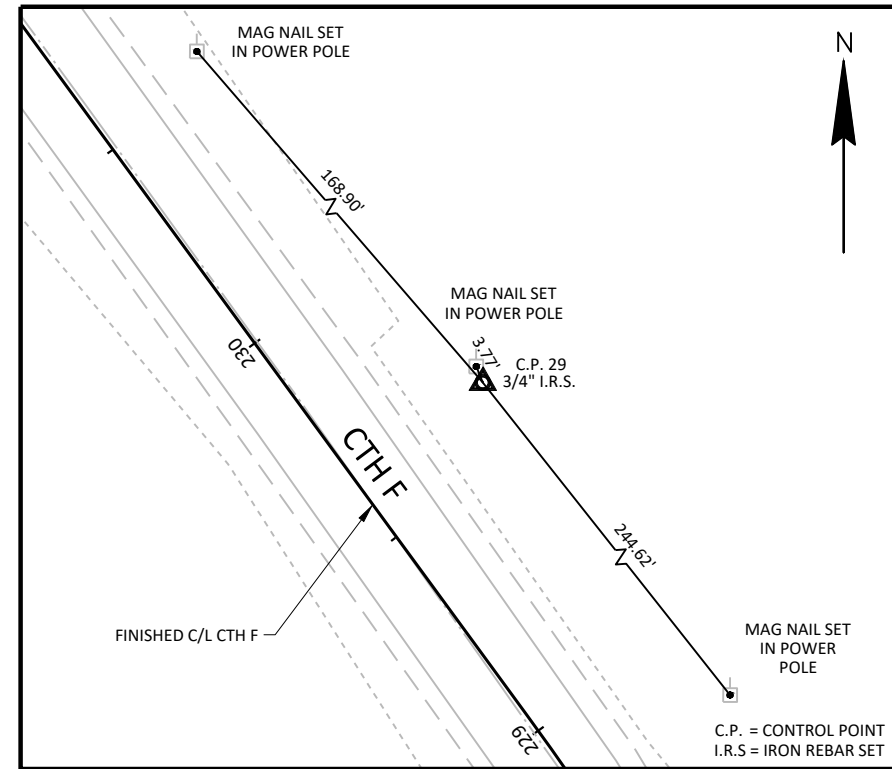
TIES TO C.P.#25

STA. 195+19.34, 36.00' RT
 Y = 116,190.06
 X = 445,410.56



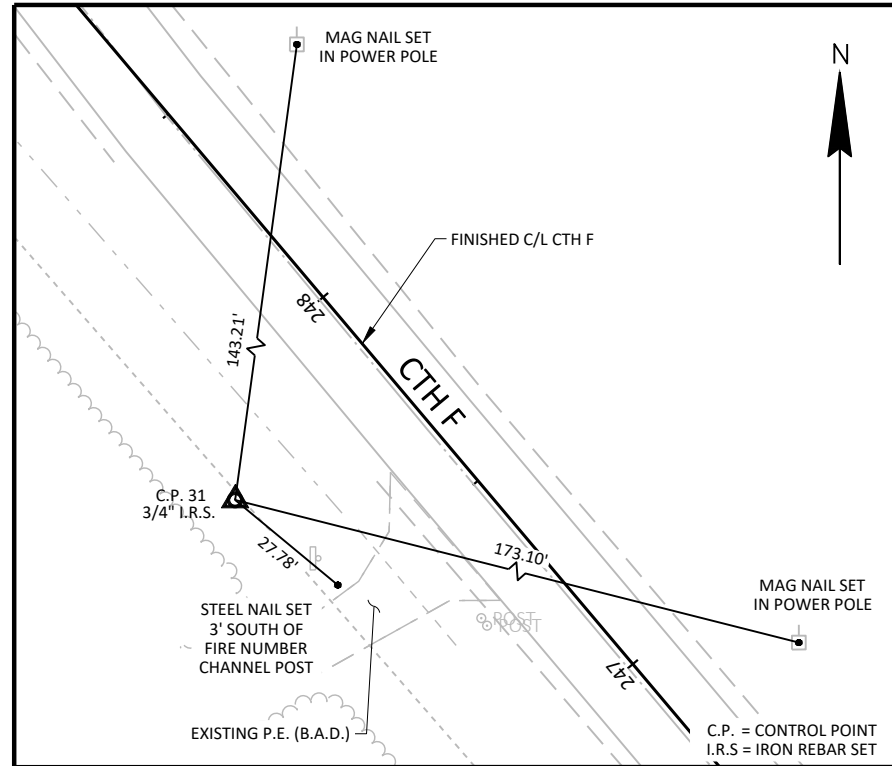
TIES TO C.P.#27

STA. 209+57.85, 46.83' LT
Y = 117,592.17
X = 445,079.24



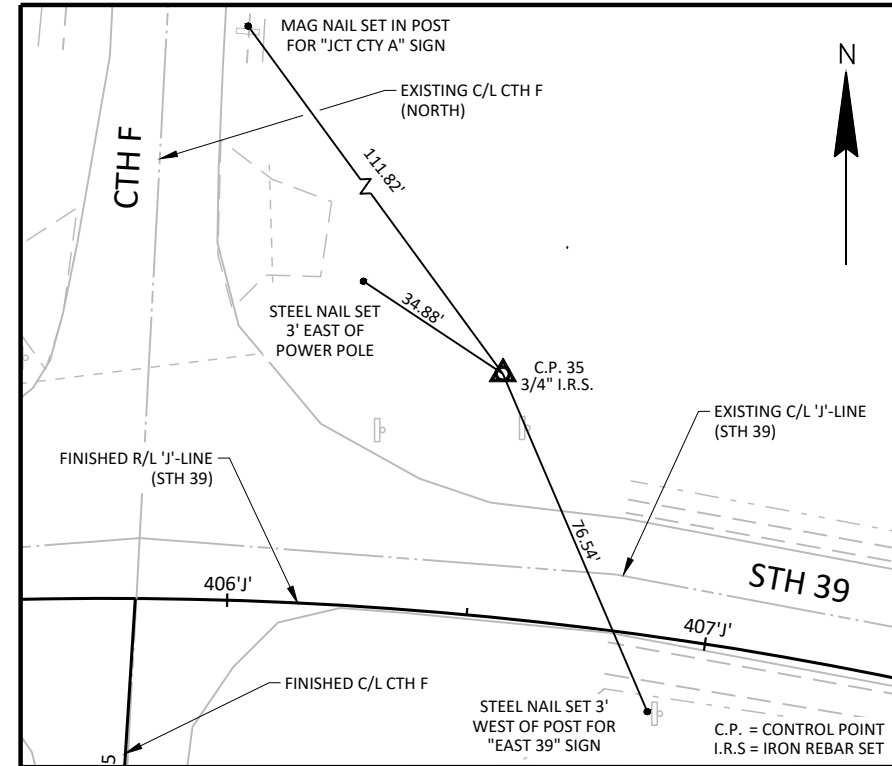
TIES TO C.P.#29

STA. 229+65.32, 33.52' RT
Y = 119,584.22
X = 444,905.67



TIES TO C.P.#31

STA. 247+79.57, 41.22' LT
Y = 120,954.15
X = 443,714.60



TIES TO C.P.#35

STA. 406'J'+52.76, 50.69' LT
Y = 122,692.77
X = 443,778.09

Estimate Of Quantities

5034-00-72

Line	Item	Item Description	Unit	Total	Qty
0002	201.0105	Clearing	STA	35.000	35.000
0004	201.0120	Clearing	ID	42.000	42.000
0006	201.0205	Grubbing	STA	35.000	35.000
0008	201.0220	Grubbing	ID	95.000	95.000
0010	203.0100	Removing Small Pipe Culverts	EACH	35.000	35.000
0012	203.0220	Removing Structure (structure) 01. 99+91	EACH	1.000	1.000
0014	203.0220	Removing Structure (structure) 02. 212+96	EACH	1.000	1.000
0016	204.0100	Removing Concrete Pavement	SY	44.000	44.000
0018	204.0155	Removing Concrete Sidewalk	SY	3.000	3.000
0020	204.0180	Removing Delineators and Markers	EACH	32.000	32.000
0022	204.9090.S	Removing (item description) 01. Removing Retaining Wall	LF	50.000	50.000
0024	205.0100	Excavation Common	CY	55,190.000	55,190.000
0026	205.0200	Excavation Rock	CY	1,600.000	1,600.000
0028	205.0400	Excavation Marsh	CY	2,100.000	2,100.000
0030	208.0100	Borrow	CY	45,785.000	45,785.000
0032	213.0100	Finishing Roadway (project) 01. 5034-00-72	EACH	1.000	1.000
0034	305.0110	Base Aggregate Dense 3/4-Inch	TON	11,450.000	11,450.000
0036	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	76,500.000	76,500.000
0038	311.0110	Breaker Run	TON	33,110.000	33,110.000
0040	312.0110	Select Crushed Material	TON	6,200.000	6,200.000
0042	325.0100	Pulverize and Relay	SY	45,100.000	45,100.000
0044	416.0160	Concrete Driveway 6-Inch	SY	210.000	210.000
0046	416.1010	Concrete Surface Drains	CY	5.000	5.000
0048	450.4000	HMA Cold Weather Paving	TON	5,030.000	5,030.000
0050	455.0605	Tack Coat	GAL	4,020.000	4,020.000
0052	460.2000	Incentive Density HMA Pavement	DOL	12,880.000	12,880.000
0054	460.6223	HMA Pavement 3 MT 58-28 S	TON	11,500.000	11,500.000
0056	460.6224	HMA Pavement 4 MT 58-28 S	TON	8,620.000	8,620.000
0058	465.0315	Asphaltic Flumes	SY	110.000	110.000
0060	465.0450	Asphaltic Intersection Rumble Strips	SY	75.000	75.000
0062	521.1018	Apron Endwalls for Culvert Pipe Steel 18-Inch	EACH	26.000	26.000
0064	521.1024	Apron Endwalls for Culvert Pipe Steel 24-Inch	EACH	33.000	33.000
0066	521.1030	Apron Endwalls for Culvert Pipe Steel 30-Inch	EACH	2.000	2.000
0068	521.1235	Apron Endwalls for Pipe Arch Steel 35x24-Inch	EACH	4.000	4.000
0070	521.1283	Apron Endwalls for Pipe Arch Steel 83x57-Inch	EACH	2.000	2.000
0072	521.1900	Pipe Cattle Pass Corrugated Steel	LF	186.000	186.000
0074	521.3118	Culvert Pipe Corrugated Steel 18-Inch	LF	538.000	538.000
0076	521.3124	Culvert Pipe Corrugated Steel 24-Inch	LF	1,016.000	1,016.000
0078	521.3130	Culvert Pipe Corrugated Steel 30-Inch	LF	116.000	116.000
0080	521.3735	Pipe Arch Corrugated Steel 35x24-Inch	LF	78.000	78.000
0082	521.3783	Pipe Arch Corrugated Steel 83x57-Inch	LF	82.000	82.000
0084	522.0124	Culvert Pipe Reinforced Concrete Class III 24-Inch	LF	6.000	6.000
0086	522.1012	Apron Endwalls for Culvert Pipe Reinforced Concrete 12-Inch	EACH	1.000	1.000
0088	522.1024	Apron Endwalls for Culvert Pipe Reinforced Concrete 24-Inch	EACH	2.000	2.000
0090	601.0411	Concrete Curb & Gutter 30-Inch Type D	LF	4,175.000	4,175.000
0092	601.0553	Concrete Curb & Gutter 4-Inch Sloped 36-Inch Type D	LF	2,255.000	2,255.000
0094	601.0557	Concrete Curb & Gutter 6-Inch Sloped 36-Inch Type D	LF	490.000	490.000
0096	606.0300	Riprap Heavy	CY	490.000	490.000
0098	608.0005	Storm Sewer Rock Excavation	CY	765.000	765.000

Estimate Of Quantities

5034-00-72

Line	Item	Item Description	Unit	Total	Qty
0100	608.0412	Storm Sewer Pipe Reinforced Concrete Class IV 12-Inch	LF	465.000	465.000
0102	608.0424	Storm Sewer Pipe Reinforced Concrete Class IV 24-Inch	LF	1,354.000	1,354.000
0104	611.0535	Manhole Covers Type J-Special	EACH	6.000	6.000
0106	611.0639	Inlet Covers Type H-S	EACH	9.000	9.000
0108	611.0645	Inlet Covers Type MS-A	EACH	1.000	1.000
0110	611.2004	Manholes 4-FT Diameter	EACH	4.000	4.000
0112	611.2005	Manholes 5-FT Diameter	EACH	3.000	3.000
0114	611.3230	Inlets 2x3-FT	EACH	8.000	8.000
0116	611.3901	Inlets Median 1 Grate	EACH	1.000	1.000
0118	616.0700.S	Fence Safety	LF	485.000	485.000
0120	618.0100	Maintenance And Repair of Haul Roads (project) 01. 5034-00-72	EACH	1.000	1.000
0122	619.1000	Mobilization	EACH	1.000	1.000
0124	624.0100	Water	MGAL	1,910.000	1,910.000
0126	625.0100	Topsoil	SY	18,750.000	18,750.000
0128	625.0500	Salvaged Topsoil	SY	210,000.000	210,000.000
0130	627.0200	Mulching	SY	196,000.000	196,000.000
0132	628.1504	Silt Fence	LF	38,275.000	38,275.000
0134	628.1520	Silt Fence Maintenance	LF	153,100.000	153,100.000
0136	628.1905	Mobilizations Erosion Control	EACH	11.000	11.000
0138	628.1910	Mobilizations Emergency Erosion Control	EACH	5.000	5.000
0140	628.2002	Erosion Mat Class I Type A	SY	11,700.000	11,700.000
0142	628.2004	Erosion Mat Class I Type B	SY	32,050.000	32,050.000
0144	628.2023	Erosion Mat Class II Type B	SY	13,050.000	13,050.000
0146	628.6510	Soil Stabilizer Type B	ACRE	48.000	48.000
0148	628.7005	Inlet Protection Type A	EACH	10.000	10.000
0150	628.7015	Inlet Protection Type C	EACH	9.000	9.000
0152	628.7504	Temporary Ditch Checks	LF	2,030.000	2,030.000
0154	628.7555	Culvert Pipe Checks	EACH	200.000	200.000
0156	629.0210	Fertilizer Type B	CWT	168.000	168.000
0158	630.0120	Seeding Mixture No. 20	LB	5,725.000	5,725.000
0160	630.0140	Seeding Mixture No. 40	LB	875.000	875.000
0162	630.0160	Seeding Mixture No. 60	LB	10.000	10.000
0164	630.0200	Seeding Temporary	LB	7,000.000	7,000.000
0166	630.0300	Seeding Borrow Pit	LB	950.000	950.000
0168	630.0500	Seed Water	MGAL	6,535.000	6,535.000
0170	633.5200	Markers Culvert End	EACH	41.000	41.000
0172	634.0614	Posts Wood 4x6-Inch X 14-FT	EACH	8.000	8.000
0174	634.0616	Posts Wood 4x6-Inch X 16-FT	EACH	55.000	55.000
0176	637.2210	Signs Type II Reflective H	SF	198.760	198.760
0178	637.2230	Signs Type II Reflective F	SF	241.460	241.460
0180	638.2602	Removing Signs Type II	EACH	75.000	75.000
0182	638.3000	Removing Small Sign Supports	EACH	67.000	67.000
0184	642.5001	Field Office Type B	EACH	1.000	1.000
0186	643.0300	Traffic Control Drums	DAY	6,500.000	6,500.000
0188	643.0420	Traffic Control Barricades Type III	DAY	4,200.000	4,200.000
0190	643.0705	Traffic Control Warning Lights Type A	DAY	6,900.000	6,900.000
0192	643.0900	Traffic Control Signs	DAY	7,300.000	7,300.000
0194	643.0920	Traffic Control Covering Signs Type II	EACH	11.000	11.000
0196	643.1050	Traffic Control Signs PCMS	DAY	35.000	35.000

Estimate Of Quantities

5034-00-72

Line	Item	Item Description	Unit	Total	Qty
0198	643.5000	Traffic Control	EACH	1.000	1.000
0200	645.0120	Geotextile Type HR	SY	1,130.000	1,130.000
0202	645.0135	Geotextile Type SR	SY	3,300.000	3,300.000
0204	646.6120	Marking Stop Line Epoxy 18-Inch	LF	90.000	90.000
0206	646.6464	Cold Weather Marking Epoxy 4-Inch	LF	95,950.000	95,950.000
0208	646.6468	Cold Weather Marking Epoxy 8-Inch	LF	500.000	500.000
0210	646.9000	Marking Removal Line 4-Inch	LF	2,300.000	2,300.000
0212	648.0100	Locating No-Passing Zones	MI	4.830	4.830
0214	650.4000	Construction Staking Storm Sewer	EACH	18.000	18.000
0216	650.4500	Construction Staking Subgrade	LF	29,900.000	29,900.000
0218	650.5000	Construction Staking Base	LF	29,900.000	29,900.000
0220	650.5500	Construction Staking Curb Gutter and Curb & Gutter	LF	6,910.000	6,910.000
0222	650.6000	Construction Staking Pipe Culverts	EACH	20.000	20.000
0224	650.9910	Construction Staking Supplemental Control (project) 01. 5034-00-72	LS	1.000	1.000
0226	650.9910	Construction Staking Supplemental Control (project) 02. 5034-00-72	LS	1.000	1.000
0228	650.9920	Construction Staking Slope Stakes	LF	29,900.000	29,900.000
0230	690.0150	Sawing Asphalt	LF	1,520.000	1,520.000
0232	690.0250	Sawing Concrete	LF	5.000	5.000
0234	740.0440	Incentive IRI Ride	DOL	19,320.000	19,320.000
0236	999.1501.S	Crack and Damage Survey	EACH	1.000	1.000
0238	ASP.1T0A	On-the-Job Training Apprentice at \$5.00/HR	HRS	2,160.000	2,160.000
0240	ASP.1T0G	On-the-Job Training Graduate at \$5.00/HR	HRS	4,560.000	4,560.000
0242	SPV.0035	Special 01. Temporary Stone Ditch Checks	CY	14.000	14.000
0244	SPV.0060	Special 01. Construction Staking Sedimentation Basin	EACH	1.000	1.000
0246	SPV.0060	Special 02. Joint Ties	EACH	134.000	134.000
0248	SPV.0060	Special 03. Reestablish Section Corner Monument	EACH	2.000	2.000
0250	SPV.0090	Special 01. PVC Pipe 4-Inch	LF	43.000	43.000
0252	SPV.0180	Special 01. Sedimentation Basin Layer	SY	530.000	530.000
0254	SPV.0195	Special 01. Limestone Screenings	TON	15.000	15.000

CLEARING & GRUBBING

STATION - STATION	LOCATION	201.0105 CLEARING (CAT 010)		201.0120 CLEARING (CAT 010) (CAT 020)		201.0205 GRUBBING (CAT 010)		201.0220 GRUBBING (CAT 010) (CAT 020)	
		(STA)	(ID)	(ID)	(STA)	(ID)	(ID)	(ID)	
22+00 - 30+00	MAINLINE	8	-	-	-	8	-	-	-
400'C'+45	'C'-LINE (P.E.), RT.	-	10	-	-	-	10	-	-
400'D'+50 - 401'D'+50	'D'-LINE (SHARED USE ACCESS), RT.	1	-	-	-	1	-	-	-
73+00 - 75+00	MAINLINE	2	-	-	-	2	-	-	-
80+50 - 82+50	MAINLINE	2	-	-	-	2	-	-	-
89+00 - 90+00	MAINLINE, LT.	1	-	-	-	1	-	-	-
90+31	MAINLINE, RT.	-	8	-	-	-	8	-	-
92+27	MAINLINE, RT.	-	7	-	-	-	7	-	-
109+00 - 114+00	MAINLINE, LT.	5	-	-	-	5	-	-	-
402'G'+00 - 406'G'+64.33	'G'-LINE (DRIVEWAY REALIGNMENT)	5	-	-	-	5	-	-	-
130+50 - 135+50	MAINLINE, LT.	5	-	-	-	5	-	-	-
140+00 - 142+00	MAINLINE, LT.	2	-	-	-	2	-	-	-
402'H'+00 - 404'H'+00	'H'-LINE (MCKENNA ROAD), LT.	2	-	-	-	2	-	-	-
173+00 - 175+00	MAINLINE	2	-	-	-	2	-	-	-
237+07	MAINLINE, RT.	-	-	9	-	-	-	-	9
255+23	MAINLINE, RT.	-	-	-	-	-	-	-	53
258+55	MAINLINE, RT.	-	-	8	-	-	-	-	8
CATEGORY TOTAL =		35	25	17		35	25	70	
PROJECT TOTAL =		35	42			35	95		

REMOVING SMALL PIPE CULVERTS

STATION	LOCATION	DESCRIPTION	203.0100 REMOVING SMALL PIPE CULVERTS		*204.0180 REMOVING DELINEATORS AND MARKERS	
			(CAT 010) (EACH)	(CAT 020) (EACH)	(CAT 010) (EACH)	(CAT 020) (EACH)
10+35	MAINLINE	18" CMCP; L=49'	1	-	-	-
26+23	MAINLINE, LT.	15" CMCP; L=22'	1	-	-	-
27+94	MAINLINE, LT.	15" CMCP; L=28'	1	-	-	-
30+57	MAINLINE, LT.	15" CMCP; L=21'	1	-	-	-
42+22	MAINLINE	36" CMCP; L=51'	1	-	2	-
43+76	MAINLINE, LT.	24" CMCP; L=25'	1	-	-	-
49+92	MAINLINE, RT.	18" CMCP; L=48'	1	-	-	-
50+75	MAINLINE, LT.	18" CMCP; L=31'	1	-	-	-
53+58	MAINLINE, RT.	18" CMCP; L=28'	1	-	-	-
53+90	MAINLINE, LT.	18" CMCP; L=35'	1	-	-	-
57+74	MAINLINE, RT.	18" CMCP; L=33'	1	-	-	-
65+84	MAINLINE	24" CMCP; L=37'	1	-	2	-
75+52	MAINLINE	24" CMCP; L=41'	1	-	2	-
91+62	MAINLINE	18" CMCP; L=38'	1	-	2	-
108+34	MAINLINE	18" CMCP; L=37'	1	-	2	-
117+71	MAINLINE	18" CMCP; L=41'	1	-	2	-
126+27	MAINLINE, LT.	18" CMCP; L=31'	1	-	-	-
132+76	MAINLINE	18" CMCP; L=62'	1	-	2	-
142+47	MAINLINE, LT.	18" CMCP; L=25'	1	-	-	-
154+64	MAINLINE	18" CMCP; L=52'	1	-	2	-
188+81	MAINLINE	18" CMCP; L=47'	1	-	2	-
193+86	MAINLINE, RT.	15" CMCP; L=71'	1	-	-	-
212+53	MAINLINE, LT.	15" CMCP; L=26'	1	-	-	-
226+65	MAINLINE	18" CMCP; L=42'	-	1	-	2
236+52	MAINLINE, RT.	18" CMCP; L=30'	-	1	-	-
237+77	MAINLINE	18" CMCP; L=47'	-	1	-	2
247+45	MAINLINE, LT.	24" CMCP; L=32'	-	1	-	-
251+39	MAINLINE, LT.	12" CMCP; L=23'	-	1	-	-
263+07	MAINLINE, RT.	18" CMCP; L=39'	-	1	-	-
264+92	MAINLINE	18" CMCP; L=47'	-	1	-	2
400'C'+55	'C'-LINE (PRIVATE ENTRANCE)	18" CMCP; L=56'	1	-	-	-
403'E'+12	'E'-LINE (HORSESHOE BEND ROAD)	18" CMCP; L=58'	1	-	-	-
401'H'+44	'H'-LINE (MCKENNA ROAD), LT.	12" CMCP; L=30'	1	-	-	-
402'H'+10	'H'-LINE (MCKENNA ROAD)	18" CMCP; L=44'	1	-	2	-
405'I'+10	'I'-LINE (E PECATONICA ROAD)	18" CMCP; L=32'	-	1	-	2
CATEGORY TOTAL =			27	6	20	8
PROJECT TOTAL =			35		28	

REMOVING STRUCTURE

STATION	LOCATION	DESCRIPTION	203.0220 REMOVING STRUCTURE (CAT 010) (EACH)	* 204.0180 REMOVING DELINEATORS AND MARKERS (CAT 010) (EACH)
99+91	MAINLINE	CATTLE PASS; L=63'	1	2
212+96	MAINLINE	CATTLE PASS; L=75'	1	2
TOTAL =			2	4

REMOVING CONCRETE PAVEMENT

STATION - STATION	LOCATION	204.0100 (CAT 020) (SY)
255+35 - 255+55	MAINLINE, RT.	44
TOTAL =		44

REMOVING CONCRETE SIDEWALK

STATION - STATION	LOCATION	204.0155 (CAT 010) (SY)
25+57	MAINLINE, RT.	3
TOTAL =		3

REMOVING RETAINING WALL

STATION - STATION	LOCATION	204.9090.S (CAT 010) (LF)
142+21 - 142+36	MAINLINE, LT.	29
142+58 - 142+63	MAINLINE, LT.	21
TOTAL =		50

PULVERIZE AND RELAY

STATION - STATION	LOCATION	325.0100 PULVERIZE AND RELAY	
		(CAT 010) (SY)	(CAT 020) (SY)
50+00 - 168+73	MAINLINE	30,450	-
179+70 - 186+50	MAINLINE	1,750	-
191+00 - 211+10	MAINLINE	5,100	-
226+80 - 259+25	MAINLINE	-	7,800
CATEGORY TOTALS =		37,300	7,800
PROJECT TOTAL =		45,100	

CONCRETE DRIVEWAY 6-INCH

STATION	LOCATION	416.0160	
		(CAT 010) (SY)	(CAT 020) (SY)
15+00	MAINLINE, LT.	15	-
15+00	MAINLINE, RT.	15	-
23+10	MAINLINE, LT.	15	-
26+15	MAINLINE, RT.	23	-
26+23	MAINLINE, LT.	13	-
27+50	MAINLINE, LT.	11	-
30+52	MAINLINE, LT.	10	-
53+58	MAINLINE, RT.	18	-
57+74	MAINLINE, RT.	17	-
172+04	MAINLINE, LT.	11	-
174+15	MAINLINE, LT.	13	-
260+36	MAINLINE, LT.	-	10
261+26	MAINLINE, RT.	-	18
400'B+13	MAINLINE, LT.	21	-
CATEGORY TOTALS =		182	28
PROJECT TOTAL =		210	

ASPHALTIC FLUMES

STATION	LOCATION	465.0315	
		(CAT 010) (SY)	(CAT 020) (SY)
403'A'+65	'A'-LINE (STH 78), LT.	10	-
29+50	MAINLINE, RT.	11	-
30+75	MAINLINE, LT.	10	-
53+30	MAINLINE, RT.	12	-
109+00	MAINLINE, LT.	8	-
114+00	MAINLINE, LT.	13	-
403'H'+34	'H'-LINE (MCKENNA ROAD), LT.	8	-
174+50	MAINLINE, LT.	12	-
259+25	MAINLINE, RT.	-	12
261+50	MAINLINE, LT.	-	6
405'J'+00	'J'-LINE (STH 39), RT.	-	8
CATEGORY TOTALS =		84	26
PROJECT TOTALS =		110	

SELECT CRUSHED/GEOTEXTILE TYPE SR

STATION - STATION	LOCATION	312.0110 SELECT CRUSHED MATERIAL (CAT 010) (TON)	645.0135 GEOTEXTILE TYPE SR (CAT 010) (SY)
		36+62 - 43+02 / 36+77 - 43+88	MAINLINE, LT. & RT.
TOTAL =		6,200	3,300

ASPHALTIC INTERSECTION RUMBLE STRIP

STATION - STATION	LOCATION	465.0450 (CAT 020) (SY)
255+24 - 255+49	MAINLINE, RT	25
258+74 - 258+99	MAINLINE, RT	25
260+99 - 261+24	MAINLINE, RT	25
PROJECT TOTAL =		75

CONCRETE SURFACE DRAINS

STATION	LOCATION	416.1010	
		(CAT 010) (CY)	(CAT 020) (CY)
10+70	MAINLINE, LT.	3	-
264+73	MAINLINE, RT.	-	2
CATEGORY TOTALS =		3	2
PROJECT TOTAL =		5	

EARTHWORK SUMMARY

STATION - STATION	LOCATION	(1) 205.0100 EXCAVATION COMMON		UNEXPANDED FILL		(2) 205.0200 EXCAVATION ROCK	(3) EXPANDED ROCK FACTOR	(4) 205.0400 EXCAVATION MARSH	(5) REDUCED MARSH FACTOR	(6) EXPANDED FILL FACTOR 1.25		(7) MASS ORDINATE +/-		208.0100 BORROW	
		(CAT 010)	(CAT 020)	(CAT 010)	(CAT 020)	(CAT 010)	(CY)	(CAT 010)	(CY)	(CAT 010)	(CAT 020)	(CAT 010)	(CAT 020)	(CAT 010)	(CAT 020)
		(CY)	(CY)	(CY)	(CY)	(CY)	1.1	(CY)	0.6	(CY)	(CY)	(CY)	(CY)	(CY)	(CY)
10+24 - 226+00	MAINLINE	36130	0	52380	0	1600	1760	2100	1260	61700	0	-25570	0	25570	0
226+00 - 265+24.10	MAINLINE	0	3670	0	8500	-	-	-	-	0	10625	0	-6955	0	6955
399'A+55 - 406'A+98.30	'A'-LINE (STH 78)	1035	0	60	0	-	-	-	-	75	0	960	0	-960	0
399'B+62.87 - 402'B+03.52	'B'-LINE (MOUND STREET)	1070	0	65	0	-	-	-	-	80	0	990	0	-990	0
400'C+00 - 401'C+68.23	'C'-LINE (P.E.)	1070	0	20	0	-	-	-	-	25	0	1045	0	-1045	0
400'D+00 - 402'D+87.03	'D'-LINE (SHARED USE ACCESS)	910	0	200	0	-	-	-	-	250	0	660	0	-660	0
401'E+88 - 403'E+24.08	'E'-LINE (HORSESHOE BEND ROAD)	520	0	15	0	-	-	-	-	20	0	500	0	-500	0
400'F+16 - 402'F+20	'F'-LINE (WEST MOSCOW ROAD)	275	0	90	0	-	-	-	-	115	0	160	0	-160	0
400'G+15 - 406'G+64.33	'G'-LINE (DRIVEWAY REALIGNMENT)	1230	0	65	0	-	-	-	-	80	0	1150	0	-1150	0
400'H+20 - 404'H+28.79	'H'-LINE (MCKENNA ROAD)	520	0	2590	0	-	-	-	-	3240	0	-2720	0	2720	0
402'I+50 - 409'I+24.87	'I'-LINE (EAST PECATONICA ROAD)	0	350	0	16980	-	-	-	-	0	21225	0	-20875	0	20875
400'J+50 - 407'J+47.45	'J'-LINE (STH 39)	0	720	0	480	-	-	-	-	0	600	0	120	0	-120
1'N+00 - 2'N+20	'N'-LINE (CATTLE WALKWAY)	970	0	0	0	-	-	-	-	0	0	970	0	-970	0
-	P.E./C.E./F.E.	3900	800	1950	400	-	-	-	-	2440	500	1460	300	-1460	-300
-	STORMWATER POND	2020	0	0	0	-	-	-	-	0	0	2020	0	-2020	0
CATEGORY TOTALS =		49650	5540	57435	26360	1600	1760	2100	1260	68025	32950	-18375	-27410	18375	27410
PROJECT TOTALS =		55190		83795		1600	1760	2100	1260	100975		-45785		45785	

NOTES:

- 1.) COMMON EXCAVATION IS THE SUM OF THE CUT, ITEM NUMBER 205.0100.
 - 2.) ROCK EXCAVATION ITEM NUMBER 205.0200
 - 3.) EXPANDED ROCK - FACTOR = 1.1.
 - 4.) EXCAVATION MARSH - TO BE BACKFILLED WITH SELECT CRUSHED MATERIAL. ITEM 205.0400.
 - 5.) REDUCED MARSH IN FILL - EXCAVATED MARSH MATERIAL IS USABLE IN FILLS OUTSIDE THE 2:1 SLOPE. MARSH IN FILL REDUCTION FACTOR = 0.6.
 - 6.) FILL EXPANDED = (UNEXPANDED FILL - EXPANDED ROCK (1.1) - REDUCED MARSH (0.6)) * 1.25
 - 7.) THE MASS ORDINATE + OR - QTY CALCULATED FOR THE DIVISION. PLUS QUANTITY INDICATES AN EXCESS OF MATERIAL WITHIN THE DIVISION. MINUS INDICATES A SHORTAGE OF MATERIAL WITHIN THE DIVISION
- NOTE: EARTHWORK QUANTITIES FOR THE MAINLINE INCLUDE THE QUANTITIES FOR ALIGNMENTS 'K', 'L', 'M', 'O', 'P', 'Q', AND 'R'-LINES.

BASE AGGREGATE DENSE / BREAKER RUN / LIMESTONE SCREENINGS

STATION - STATION	LOCATION	305.0110 BASE AGGREGATE DENSE 3/4-INCH		305.0120 BASE AGGREGATE DENSE 1 1/4-INCH		311.0110 BREAKER RUN		SPV.0195.01 LIMESTONE SCREENINGS
		(CAT 010)	(CAT 020)	(CAT 010)	(CAT 020)	(CAT 010)	(CAT 020)	(CAT 010)
		(TON)	(TON)	(TON)	(TON)	(TON)	(TON)	(TON)
10+24 - 50+00	MAINLINE	550	-	10,020	-	12,010	-	-
50+00 - 168+73	MAINLINE	3,250	-	33,500	-	-	-	-
168+73 - 179+70	MAINLINE	280	-	2,650	-	3,500	-	-
179+70 - 186+50	MAINLINE	190	-	2,020	-	-	-	-
186+50 - 191+00	MAINLINE	125	-	1,310	-	1,430	-	-
191+00 - 211+10	MAINLINE	550	-	5,670	-	-	-	-
211+10 - 226+00	MAINLINE	410	-	4,340	-	4,730	-	-
226+00 - 226+80	MAINLINE	-	20	-	240	-	260	-
226+80 - 257+50	MAINLINE	-	840	-	8,660	-	-	-
257+50 - 265+24.10	MAINLINE	-	55	-	1,870	-	2,040	-
399'A+55 - 406'A+98.30	'A'-LINE (STH 78)	21	-	440	-	580	-	-
399'B+62.87 - 402'B+03.52	'B'-LINE (MOUND STREET)	3	-	600	-	670	-	-
400'C+00 - 401'C+68.23	'C'-LINE (P.E.)	235	-	-	-	295	-	-
400'D+00 - 402'D+87.03	'D'-LINE (SHARED USE ACCESS)	385	-	-	-	395	-	-
401'E+88 - 403'E+24	'E'-LINE (HORSESHOE BEND ROAD)	42	-	610	-	-	-	-
402'F+20 - 400'F+16	'F'-LINE (WEST MOSCOW ROAD)	51	-	710	-	-	-	-
400'G+15 - 406'G+52.3	'G'-LINE (DRIVEWAY REALIGNMENT)	515	-	-	-	830	-	-
400'H+20 - 404'H+28.79	'H'-LINE (MCKENNA ROAD)	68	-	1,210	-	-	-	-
402'I+50 - 409'I+24.87	'I'-LINE (EAST PECATONICA ROAD)	-	120	-	1,820	-	-	-
400'J+50 - 407'J+47.45	'J'-LINE (STH 39)	-	30	-	680	-	890	-
500'K+16 - 500'K+75	'K'-LINE (P.E.)	75	-	-	-	93	-	-
500'L+00 - 501'L+22.68	'L'-LINE (F.E.)	120	-	-	-	195	-	-
500'M+00 - 501'M+26.13	'M'-LINE (F.E.)	120	-	-	-	195	-	-
1'N+00 - 2'N+20	'N'-LINE (CATTLE WALKWAY)	-	-	-	-	25	-	15
10'O+20 - 10'O+61.86	'O'-LINE (NORTH MAIN STREET)	-	-	150	-	165	-	-
1'P+00 - 1'P+85.75	'P'-LINE (P.E.)	92	-	-	-	95	-	-
1'Q+00 - 2'Q+89.17	'Q'-LINE (F.E.)	192	-	-	-	291	-	-
1'R+00 - 1'R+90.97	'R'-LINE (F.E.)	112	-	-	-	142	-	-
PROJECT	CULVERT REPLACEMENT	-	-	-	-	450	60	-
PROJECT	DRIVEWAYS (P.E./F.E./C.E.)	2,439	560	-	-	3,059	710	-
CATEGORY TOTALS =		9,825	1,625	63,230	13,270	29,150	3,960	15
PROJECT TOTALS =		11,450		76,500		33,110		15

HMA PAVEMENT

STATION - STATION	LOCATION	455.0605 TACK COAT		460.6223 HMA PAVEMENT 3 MT 58-28 S		460.6224 HMA PAVEMENT 4 MT 58-28 S	
		(CAT 010)	(CAT 020)	(CAT 010)	(CAT 020)	(CAT 010)	(CAT 020)
		(GAL)	(GAL)	(TON)	(TON)	(TON)	(TON)
10+24 - 30+75	MAINLINE	380	-	1,090	-	760	-
30+75 - 50+00	MAINLINE	270	-	770	-	540	-
50+00 - 226+00	MAINLINE	2,434	-	7,000	-	4,900	-
226+00 - 257+50	MAINLINE	-	440	-	1,255	-	880
257+50 - 265+24.10	MAINLINE	-	130	-	360	-	250
399'A+55 - 406'A+98.30	'A'-LINE (STH 78)	25	-	70	-	50	-
399'B+62.87 - 402'B+03.52	'B'-LINE (MOUND STREET)	46	-	130	-	90	-
400'C+00 - 401'C+68.23	'C'-LINE (P.E.)	-	-	-	-	45	-
400'D+12 - 402'D+87.03	'D'-LINE (SHARED USE ACCESS)	-	-	-	-	50	-
401'E+88 - 403'E+24	'E'-LINE (HORSESHOE BEND ROAD)	35	-	100	-	70	-
402'F+20 - 400'F+16	'F'-LINE (WEST MOSCOW ROAD)	40	-	110	-	75	-
400'H+20 - 404'H+28.79	'H'-LINE (MCKENNA ROAD)	65	-	185	-	130	-
402'I+50 - 409'I+24.87	'I'-LINE (EAST PECATONICA ROAD)	-	95	-	270	-	190
400'J+50 - 407'J+47.45	'J'-LINE (STH 39)	-	45	-	120	-	85
500'K+16 - 500'K+75	'K'-LINE (P.E.)	-	-	-	-	-	20
10'O+20 - 10'O+61.86	'O'-LINE (NORTH MAIN STREET)	15	-	40	-	30	-
1'P+00 - 1'P+85.75	'P'-LINE (P.E.)	-	-	-	-	20	-
1'R+00 - 1'R+90.97	'R'-LINE (F.E.)	-	-	-	-	30	-
PROJECT	DRIVEWAYS (P.E./F.E./C.E.)	-	-	-	-	350	55
CATEGORY TOTALS =		3,310	710	9,495	2,005	7,140	1,480
PROJECT TOTALS =		4,020		11,500		8,620	

CULVERT PIPES (CIRCULAR)

STATION	LOCATION	521.1018		521.3118		521.1024		521.3124		521.1030	521.3130	522.0124	* 522.1024	* 633.5200		* 650.6000	
		APRON ENDWALLS FOR CORRUGATED STEEL 18-INCH		CORRUGATED STEEL 18-INCH		APRON ENDWALLS CORRUGATED STEEL 24-INCH		CORRUGATED STEEL 24-INCH		APRON ENDWALLS CORRUGATED STEEL 30-INCH	CORRUGATED STEEL 30-INCH	REINFORCED CONCRETE CLASS III 24-INCH	APRON ENDWALLS FOR CULVERT PIPE REINFORCED CONCRETE 24-INCH	MARKERS CULVERT END		CONSTRUCTION STAKING PIPE CULVERTS	
		(CAT 010) (EACH)	(CAT 020) (EACH)	(CAT 010) (LF)	(CAT 020) (LF)	(CAT 010) (EACH)	(CAT 020) (EACH)	(CAT 010) (LF)	(CAT 020) (LF)	(CAT 020) (LF)	(CAT 020) (LF)	(CAT 010) (LF)	(CAT 010) (LF)	(CAT 010) (EACH)	(CAT 020) (EACH)	(CAT 010) (EACH)	(CAT 020) (EACH)
405'A+72	'A'-LINE (STH 78), LT.	-	-	-	-	-	-	-	-	-	-	6	1	1	-	-	-
402'D+30	'D'-LINE (SHARED USE ACCESS)	2	-	46	-	-	-	-	-	-	-	-	-	-	-	1	-
401'E+63	'E'-LINE (HORSESHOE BEND ROAD), LT	2	-	36	-	-	-	-	-	-	-	-	-	-	-	-	-
403'E+09	'E'-LINE (HORSESHOE BEND ROAD)	-	-	-	-	2	-	102	-	-	-	-	-	2	-	1	-
401'H+44	'H'-LINE (MCKENNA ROAD), LT.	2	-	36	-	-	-	-	-	-	-	-	-	-	-	-	-
402'H+00	'H'-LINE (MCKENNA ROAD)	-	-	-	-	2	-	48	-	-	-	-	-	2	-	1	-
402'I+00	'I'-LINE (EAST PECATONICA ROAD), RT	-	2	-	44	-	-	-	-	-	-	-	-	-	-	-	-
405'I+00	'I'-LINE (EAST PECATONICA ROAD)	-	-	-	-	-	-	-	-	2	116	-	-	2	-	-	1
402'G+25	'G'-LINE (DRIVEWAY REALIGNMENT)	2	-	26	-	-	-	-	-	-	-	-	-	-	-	1	-
401'J+51	'J'-LINE (STH 39)	-	-	-	-	-	1	-	8	-	-	-	-	-	1	-	-
65+84	MAINLINE	-	-	-	-	2	-	60	-	-	-	-	-	2	-	1	-
75+52	MAINLINE	-	-	-	-	2	-	60	-	-	-	-	-	2	-	1	-
91+62	MAINLINE	-	-	-	-	2	-	54	-	-	-	-	-	2	-	1	-
99+91	MAINLINE	-	-	-	-	2	-	80	-	-	-	-	-	2	-	1	-
108+34	MAINLINE	-	-	-	-	2	-	52	-	-	-	-	-	2	-	1	-
117+71	MAINLINE	-	-	-	-	2	-	52	-	-	-	-	-	2	-	1	-
132+76	MAINLINE	-	-	-	-	2	-	50	-	-	-	-	-	2	-	1	-
142+47	MAINLINE, LT.	2	-	32	-	-	-	-	-	-	-	-	-	-	-	-	-
151+78	MAINLINE, LT.	2	-	32	-	-	-	-	-	-	-	-	-	-	-	-	-
154+64	MAINLINE	-	-	-	-	2	-	60	-	-	-	-	-	2	-	1	-
188+81	MAINLINE	-	-	-	-	2	-	92	-	-	-	-	-	2	-	1	-
193+81	MAINLINE, RT.	2	-	80	-	-	-	-	-	-	-	-	-	-	-	-	-
212+27	MAINLINE, LT.	-	-	-	-	2	-	50	-	-	-	-	-	-	-	-	-
221+75	MAINLINE, RT.	2	-	48	-	-	-	-	-	-	-	-	-	-	-	-	-
226+65	MAINLINE	-	-	-	-	2	-	66	-	-	-	-	-	-	2	-	1
236+52	MAINLINE, RT.	-	2	-	30	-	-	-	-	-	-	-	-	-	-	-	-
237+77	MAINLINE	-	-	-	-	-	2	-	70	-	-	-	-	-	2	-	1
247+45	MAINLINE, LT.	-	-	-	-	-	2	-	32	-	-	-	-	-	-	-	-
251+29	MAINLINE, LT.	-	2	-	34	-	-	-	-	-	-	-	-	-	-	-	-
257+15	MAINLINE, RT.	-	2	-	44	-	-	-	-	-	-	-	-	-	-	-	-
258+50	MAINLINE, RT.	-	2	-	50	-	-	-	-	-	-	-	-	-	-	-	-
264+92	MAINLINE	-	-	-	-	-	2	-	80	-	-	-	-	-	2	-	1
CATEGORY TOTALS =		16	10	336	202	26	7	826	190	2	116	6	1	23	9	13	4
PROJECT TOTAL =		26		538		33		1016		2	116	6	1	32		17	

* MORE LISTED ELSEWHERE

PIPE SIZE	MINIMUM THICKNESS (IN.)	
	STEEL	ALUMINUM
18-INCH	0.064	-
24-INCH	0.064	-
30-INCH	0.079	-

CULVERT PIPES (PIPE ARCH)

STATION	LOCATION	521.1235	521.3735	521.1283	521.3783	* 633.5200	* 650.6000
		(CAT 010) (EACH)	(CAT 010) (LF)	(CAT 010) (EACH)	(CAT 010) (LF)	(CAT 010) (EACH)	(CAT 010) (EACH)
402'D'+65	'D'-LINE (SHARED USE ACCESS)	2	50	-	-	-	-
2'Q'+65	Q'-LINE (F.E.)	2	28	-	-	-	-
42+22	MAINLINE	-	-	2	82	2	1
CATEGORY TOTALS =		4	78	2	82	2	1
PROJECT TOTAL =		4	78	2	82	2	1

* MORE LISTED ELSEWHERE

PIPE SIZE	MINIMUM THICKNESS (IN.) STEEL
35X24-INCH	0.079
83X57-INCH	0.109

CATTLE PASS

STATION	LOCATION	521.1900	*633.5200	*650.6000
		(CAT 010) (LF)	(CAT 010) (EACH)	(CAT 010) (EACH)
60+70	MAINLINE	70	2	1
212+96	MAINLINE	116	2	1
CATEGORY TOTALS =		186	4	2
PROJECT TOTALS =		186	4	2

* MORE LISTED ELSEWHERE

CONCRETE CURB & GUTTER

STATION - STATION	LOCATION	601.0411	601.0553	601.0557	650.5500	
		(CAT 010) (LF)	(CAT 010) (LF)	(CAT 020) (LF)	(CAT 010) (LF)	(CAT 020) (LF)
10+24 - 10+94	MAINLINE, RT.	-	-	101	101	-
10+24 - 10+86	MAINLINE, LT.	-	-	83	83	-
10+94 - 29+50	MAINLINE, RT.	1,866	-	-	1,856	-
10+86 - 30+75	MAINLINE, LT.	1,873	-	-	1,873	-
53+30 - 60+50	MAINLINE, RT.	-	720	-	720	-
109+00 - 114+00	MAINLINE, LT.	-	487	-	487	-
170+67 - 171+32	MAINLINE, LT.	-	-	122	122	-
171+32 - 174+50	MAINLINE, LT.	-	322	-	322	-
259+25 - 264+50	MAINLINE, RT.	-	-	528	-	528
259+50 - 261+50	MAINLINE, LT.	-	-	198	-	198
264+50 - 265+24.10	MAINLINE, RT.	-	-	-	92	92
264+61 - 265+24.10	MAINLINE, LT.	-	-	-	92	92
399'B'+92.87 - 402'B'+03.52	'B'-LINE (MOUND STREET), LT.	242	-	-	242	-
400'B'+30 - 402'B'+03.52	'B'-LINE (MOUND STREET), RT.	194	-	-	194	-
CATEGORY TOTALS =		4,175	1,529	726	306	184
PROJECT TOTALS =		4,175	2,255	490	6,000	910

RIPRAP HEAVY/GEOTEXTILE TYPE HR

STATION - STATION	LOCATION	606.0300		645.0120	
		(CAT 010) (CY)	(CAT 020) (CY)	(CAT 010) (SY)	(CAT 020) (SY)
399'A'+60 - 400'A'+72	'A'-LINE (STH 78), LT.	225	-	405	-
10+50	MAINLINE, LT.	25	-	65	-
11+50	MAINLINE, LT.	15	-	35	-
400'C'+66 - 401'C'+46	'C'-LINE (P.E.), RT.	91	-	204	-
402'D'+50	D'-LINE (SHARED USE ACCESS), LT.	4	-	14	-
42+22	MAINLINE, LT.	20	-	50	-
65+84	MAINLINE, LT.	6	-	20	-
75+52	MAINLINE, LT.	6	-	20	-
403'E'+09	'E'-LINE (HORSESHOE BEND ROAD), RT.	6	-	20	-
91+62	MAINLINE, RT.	6	-	20	-
99+93	MAINLINE, RT.	6	-	20	-
108+34	MAINLINE, RT.	6	-	20	-
402'G'+25	'G'-LINE (DRIVEWAY REALIGNMENT), LT.	4	-	14	-
117+71	MAINLINE, RT.	6	-	20	-
132+76	MAINLINE, LT.	6	-	20	-
154+64	MAINLINE, LT.	6	-	20	-
402'H'+00	'H'-LINE (MCKENNA ROAD), RT.	6	-	20	-
188+81	MAINLINE, RT.	6	-	20	-
212+27	MAINLINE, LT.	6	-	18	-
226+65	MAINLINE, LT.	-	6	-	20
237+77	MAINLINE, LT.	-	6	-	20
405'I'+00	'I'-LINE (EAST PECATONICA ROAD), RT.	-	8	-	22
264+94	MAINLINE, LT.	-	6	-	20
CATEGORY TOTALS =		464	26	1,048	82
PROJECT TOTALS =		490		1130	

STORM SEWER STRUCTURES

STRUCTURE NUMBER	STATION	LOCATION	RIM ELEVATION (CAT 010) (FT)	611.0535	611.0639	611.0645	611.2004	611.2005	611.3230	611.3901	STRUCTURE DEPTH (CAT 010) (FT)	650.4000	628.7005	628.7015
				MANHOLE COVERS TYPE J-S (CAT 010) (EACH)	INLET COVERS TYPE H-S (CAT 010) (EACH)	INLET COVERS TYPE MS-A (CAT 010) (EACH)	MANHOLES 4-FT DIAMETER (CAT 010) (EACH)	MANHOLES 5-FT DIAMETER (CAT 010) (EACH)	INLETS 2X3-FT (CAT 010) (EACH)	INLETS MEDIAN 1 GRATE (CAT 010) (EACH)		CONSTRUCTION STAKING STORM SEWER (CAT 010) (EACH)	INLET PROTECTION TYPE A (CAT 010) (EACH)	INLET PROTECTION TYPE C (CAT 010) (EACH)
MH-1	12+00	16.52' LT.	829.07	-	1	-	-	1	-	-	2.68	1	1	1
MH-2	12+00	0.00	829.41	1	-	-	1	-	-	-	2.73	1	-	-
MH-3	14+00	0.00	842.91	1	-	-	1	-	-	-	9.66	1	-	-
MH-4	16+00	0.00	860.91	1	-	-	-	1	-	-	8.66	1	-	-
MH-5	18+85	0.00	886.56	1	-	-	-	1	-	-	6.31	1	-	-
MH-6	22+50	0.00	906.45	1	-	-	1	-	-	-	10.20	1	-	-
MH-7	24+40.63	0.00	904.70	1	-	-	1	-	-	-	4.45	1	-	-
I-1	12+24	16.52' LT.	830.13	-	1	-	-	-	1	-	3.54	1	1	1
I-2	16+00	16.52' LT.	860.57	-	1	-	-	-	1	-	4.24	1	1	1
I-3	16+00	16.52' RT.	860.76	-	1	-	-	-	1	-	4.43	1	1	1
I-4	16+24	16.52' LT.	862.73	-	1	-	-	-	1	-	5.56	1	1	1
I-5	16+24	16.52' RT.	862.73	-	1	-	-	-	1	-	5.56	1	1	1
I-6	401'B'+63.10	16.52' RT.	904.70	-	1	-	-	-	1	-	3.10	1	1	1
I-7	400'B'+05	16.52' RT.	909.51	-	-	1	-	-	-	1	4.00	1	1	-
I-8	27+90	16.52' LT.	893.68	-	1	-	-	-	1	-	4.00	1	1	1
I-9	18+85	16.52' LT.	886.22	-	1	-	-	-	1	-	2.22	1	1	1
EW-1	11+49.55	100.41' LT.	825.00	-	-	-	-	-	-	-	-	1	-	-
EW-2	29+00	44.00' RT.	882.22	-	-	-	-	-	-	-	-	1	-	-
PROJECT TOTALS =				6	9	1	4	3	8	1	-	18	10	9

NOTES:
 STATION AND OFFSET OF MANHOLE AND AREA INLET STRUCTURES ARE MEASURED FROM CENTER OF STRUCTURE
 STATION AND OFFSET OF CURB INLET STRUCTURES ARE MEASURED TO FLANGE OF INLET
 ALL RIM ELEVATIONS ARE MEASURED TO THE FLANGE OF THE INLET
 STRUCTURE DEPTH (INLETS 2X3) = RIM ELEVATION - INVERT LOWEST PIPE - 6 INCHES (RINGS) - 6 INCHES (CASTING HEIGHT)
 STRUCTURE DEPTH (INLETS MEDIAN 1 GRATE WITH INLET COVER TYPE MS-A) = RIM ELEVATION - INVERT LOWEST PIPE
 STRUCTURE DEPTH (MANHOLE) = RIM ELEVATION - INVERT LOWEST PIPE - 6 INCHES (RINGS) - 9 INCHES (CASTING HEIGHT)
 STRUCTURE DEPTH (INLET COVER ON MANHOLES 4-FT/5-FT DIAMETER) = RIM ELEVATION - INVERT LOWEST PIPE - 6 INCHES (RINGS) - 6 INCHES (CASTING HEIGHT)

WATER

LOCATION	624.0100	
	(CAT 010) (MGAL)	(CAT 020) (MGAL)
PROJECT	1,628	282
CATEGORY TOTALS =	1,628	282
PROJECT TOTAL =	1,910	

MOBILIZATION EROSION CONTROL

PROJECT	628.1905 MOBILIZATION EROSION CONTROL		628.1910 MOBILIZATION EMERGENCY EROSION CONTROL	
	(CAT 010) (EACH)	(CAT 020) (EACH)	(CAT 010) (EACH)	(CAT 020) (EACH)
5034-00-72	9	2	4	1
CATEGORY TOTALS =	9	2	4	1
PROJECT TOTALS =	11		5	

STORM SEWER PIPE

PIPE NUMBER	FROM STRUCTURE	TO STRUCTURE	UPSTREAM ELEVATION	DISCHARGE ELEVATION	% SLOPE	APRON ENDWALLS FOR CULVERT PIPE REINFORCED CONCRETE		608.0005	REINFORCED CONCRETE PIPE CLASS IV STORM SEWER		SPV.0090.01	* 633.5200	SPV.0060.02 JOINT TIES (EACH)	COMMENTS
						522.1012 12-INCH (CAT 010) (EACH)	* 522.1024 24-INCH (CAT 010) (EACH)	STORM SEWER ROCK EXCAVATION (CAT 010) (CY)	608.0412 12-INCH (CAT 010) (LF)	608.0424 24-INCH (CAT 010) (LF)	PVC PIPE 4-INCH (CAT 010) (LF)	MARKERS CULVERT END (CAT 010) (EACH)		
P-1	-	-	825.00	824.50	1.16	-	-	-	-	-	43	1	-	SEDIMENTATION BASIN OUTFALL
P-2	MH-1	EW-1	825.39	825.00	0.41	-	1	-	-	98	-	1	6	
P-3	I-1	MH-1	825.59	825.39	0.95	-	-	-	24	-	-	-	-	
P-4	MH-2	MH-1	825.43	825.39	0.37	-	-	-	-	17	-	-	-	
P-5	MH-3	MH-2	832.00	825.43	3.37	-	-	15	-	200	-	-	-	
P-6	MH-4	MH-3	851.00	834.00	8.71	-	-	30	-	200	-	-	50	
P-7	I-2	MH-4	855.33	854.91	2.97	-	-	4	17	-	-	-	-	
P-8	I-3	MH-4	855.33	854.91	2.97	-	-	4	17	-	-	-	-	
P-9	I-4	I-2	856.17	855.97	0.90	-	-	6	24	-	-	-	-	
P-10	I-5	I-3	856.17	855.97	0.90	-	-	6	24	-	-	-	-	
P-11	MH-5	MH-4	879.00	853.00	9.27	-	-	85	-	285	-	-	72	
P-12	MH-6	MH-5	895.00	881.00	3.89	-	-	487	-	365	-	-	-	
P-13	MH-7	MH-6	899.00	897.00	1.08	-	-	26	-	189	-	-	-	
P-14	I-6	MH-7	900.60	899.10	2.87	-	-	7	57	-	-	-	-	
P-15	I-7	I-6	905.51	900.70	3.10	-	-	9	159	-	-	-	-	
P-16	I-8	EW-2	888.68	882.22	5.15	1	-	84	126	-	-	1	6	
P-17	I-9	MH-5	883.00	882.83	1.33	-	-	2	17	-	-	-	-	
PROJECT TOTALS =						1	1	765	465	1,354	43	3	134	

NOTES:
 PIPE LENGTHS ARE MEASURED FROM CENTER OF STRUCTURES
 ROCK EXCAVATION REQUIRED FROM STORM SEWER STRUCTURES IS INCLUDED IN STORM SEWER ROCK EXCAVATION QUANTITY
 USE CLASS B BEDDING IN AREAS OF STORM SEWER ROCK EXCAVATION
 LAYOUT OF PIPE P-1 INCLUDED IN CONSTRUCTION STAKING SEDIMENTATION BASIN BID ITEM.
 * MORE LISTED ELSEWHERE

FINISHING ITEMS

STATION - STATION	LOCATION	625.0100	625.0500		627.0200		628.6510		629.0210		630.0120		630.0140		630.0160	630.0200		630.0300		630.0500	
		TOPSOIL (CAT 010) (SY)	SALVAGED TOPSOIL (CAT 010) (SY)	(CAT 020) (SY)	MULCHING (CAT 010) (SY)	(CAT 020) (SY)	SOIL STABILIZER TYPE B (CAT 010) (ACRE)	(CAT 020) (ACRE)	FERTILIZER TYPE B (CAT 010) (CWT)	(CAT 020) (CWT)	SEEDING MIXTURE NO. 20 (CAT 010) (LB)	(CAT 020) (LB)	SEEDING MIXTURE NO. 40 (CAT 010) (LB)	(CAT 020) (LB)	SEEDING MIXTURE NO. 60 (CAT 010) (LB)	SEEDING TEMPORARY (CAT 010) (LB)	(CAT 020) (LB)	SEEDING BORROW PIT (CAT 010) (LB)	(CAT 020) (LB)	SEED WATER (CAT 010) (MGAL)	(CAT 020) (MGAL)
15+00 - 226+00	MAINLINE	14,000	116,350	-	104,000	-	-	86	-	3,950	-	252	-	8	-	-	-	-	3,510	-	
226+00 - 265+24.12	MAINLINE	-	-	20,600	-	13,000	-	-	13	-	625	-	-	-	-	-	-	-	-	520	
406'A'+98.30 - 399'A'+55	'A'-LINE (STH 78)	-	3,000	-	3,000	-	-	1	-	-	-	59	-	-	-	-	-	-	74	-	
399'B'+62.87 - 402'B'+03.52	'B'-LINE (MOUND STREET)	1,000	-	-	1,000	-	-	1	-	-	-	18	-	-	-	-	-	-	23	-	
400'C'+00 - 401'C'+68.23	'C'-LINE (P.E.)	-	2,000	-	-	-	-	1	-	-	-	36	-	-	-	-	-	-	45	-	
400'D'+00 - 402'D'+87.03	'D'-LINE (SHARED USE ACCESS)	-	1,080	-	1,080	-	-	1	-	-	-	20	-	-	-	-	-	-	25	-	
401'E'+88 - 403'E'+24.08	'E'-LINE (HORSESHOE BEND ROAD)	-	775	-	200	-	-	1	-	-	-	20	-	-	-	-	-	-	25	-	
400'F'+16 - 402'F'+20	'F'-LINE (WEST MOSCOW ROAD)	-	700	-	700	-	-	1	-	-	-	17	-	-	-	-	-	-	22	-	
400'G'+15 - 406'G'+52.30	'G'-LINE (DRIVEWAY REALIGNMENT)	-	2,850	-	1,000	-	-	2	-	-	-	52	-	-	-	-	-	-	65	-	
400'H'+20 - 404'H'+28.79	'H'-LINE (MCKENNA ROAD)	-	2,750	-	2,420	-	-	2	-	-	-	58	-	-	-	-	-	-	73	-	
402'I'+50 - 409'I'+24.87	'I'-LINE (EAST PECATONICA ROAD)	-	-	7,300	-	600	-	5	5	-	-	-	145	-	-	-	-	-	-	181	
400'J'+50 - 407'J'+50	'J'-LINE (STH 39)	-	-	1,700	-	1,700	-	1	1	-	-	-	36	-	-	-	-	-	-	45	
-	BORROW PIT	-	-	-	11,600	16,500	-	7	10	-	-	-	-	-	-	-	315	435	263	362	
-	PROJECT	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	
-	UNDISTRIBUTED	3,750	39,995	10,900	31,200	8,000	39	9	25	7	1,000	150	118	44	2	5,850	1,150	85	115	1,025	277
CATEGORY TOTALS=		18,750	169,500	40,500	156,200	39,800	39	9	132	36	4,950	775	650	225	10	5,850	1,150	400	550	5,150	1,385
PROJECT TOTALS=		18,750	210,000		196,000		48		168		5,725		875		10	7,000		950		6,535	

EROSION MAT

STATION - STATION	LOCATION	628.2002		628.2004		628.2023	
		CLASS I (CAT 010) (SY)	TYPE A (CAT 020) (SY)	CLASS I (CAT 010) (SY)	TYPE B (CAT 020) (SY)	CLASS II (CAT 010) (SY)	TYPE B (CAT 020) (SY)
10+24 - 226+00	MAINLINE	3,125	-	18,200	-	4,900	-
226+00 - 265+24.12	MAINLINE	-	2,160	-	4,600	-	800
399'A'+55 - 406'A'+98.30	'A'-LINE (STH 78)	-	-	-	-	-	-
403'A'+84	'A'-LINE (STH 78), LT.	-	-	8	-	-	-
399'B'+62.87 - 402'B'+03.52	'B'-LINE (MOUND STREET)	-	-	-	-	-	-
400'C'+00 - 401'C'+68.23	'C'-LINE (P.E.)	-	-	500	-	1,500	-
400'D'+00 - 402'D'+87.03	'D'-LINE (SHARED USE ACCESS)	-	-	-	-	-	-
29+78	MAINLINE, RT.	-	-	8	-	-	-
31+03	MAINLINE, LT.	-	-	8	-	-	-
36+72	MAINLINE, LT.	6	-	-	-	-	-
53+05	MAINLINE, RT.	-	-	8	-	-	-
60+70	MAINLINE, LT.	15	-	-	-	-	-
401'E'+36	'E'-LINE (HORSESHOE BEND ROAD), LT.	6	-	-	-	-	-
401'E'+88 - 403'E'+24.08	'E'-LINE (HORSESHOE BEND ROAD)	-	-	575	-	-	-
99+91	MAINLINE, RT.	15	-	-	-	-	-
108+75	MAINLINE, LT.	-	-	8	-	-	-
400'F'+16 - 402'F'+20	'F'-LINE (WEST MOSCOW ROAD)	-	-	-	-	-	-
400'G'+15 - 406'G'+52.30	'G'-LINE (DRIVEWAY REALIGNMENT)	-	-	1,350	-	500	-
114+30	MAINLINE, LT.	-	-	8	-	-	-
142+47	MAINLINE, LT.	6	-	-	-	-	-
151+50	MAINLINE, LT.	6	-	-	-	-	-
400'H'+20 - 404'H'+28.79	'H'-LINE (MCKENNA ROAD)	-	-	330	-	-	-
401'H'+70	'H'-LINE (MCKENNA ROAD), LT.	6	-	-	-	-	-
403'H'+32	'H'-LINE (MCKENNA ROAD), LT.	-	-	8	-	-	-
174+78	MAINLINE, LT.	-	-	8	-	-	-
193+33	MAINLINE, RT.	6	-	-	-	-	-
213+30	MAINLINE, RT.	15	-	-	-	-	-
221+40	MAINLINE, RT.	6	-	-	-	-	-
236+75	MAINLINE, RT.	-	6	-	-	-	-
402'I'+30	'I'-LINE (EAST PECATONICA ROAD), RT.	-	6	-	-	-	-
402'I'+50 - 409'I'+24.87	'I'-LINE (EAST PECATONICA ROAD)	-	3,950	-	-	2,750	-
247+25	MAINLINE, LT.	-	6	-	-	-	-
251+00	MAINLINE, LT.	-	6	-	-	-	-
257+45	MAINLINE, RT.	-	6	-	-	-	-
258+15	MAINLINE, RT.	-	6	-	-	-	-
259+00	MAINLINE, RT.	-	-	-	8	-	-
264+72	MAINLINE, RT.	-	-	-	8	-	-
401'J'+25	'J'-LINE (STH 39), RT.	-	6	-	-	-	-
404'J'+93	'J'-LINE (STH 39), RT.	-	-	-	8	-	-
	UNDISTRIBUTED	813	1,523	5,256	1,151	2,400	200
CATEGORY TOTALS =		4,025	7,675	26,275	5,775	12,050	1,000
PROJECT TOTALS =			11,700		32,050		13,050

SILT FENCE

STATION - STATION	LOCATION	628.1504 SILT FENCE		628.1520 SILT FENCE MAINTENANCE	
		(CAT 010) (LF)	(CAT 020) (LF)	(CAT 010) (LF)	(CAT 020) (LF)
400'A'+50 - 406'A'+98	MAINLINE, LT.	700	-	2,800	-
403'A'+70 - 14+87	MAINLINE, RT.	470	-	1,880	-
12+58 - 14+88	MAINLINE, LT.	225	-	900	-
15+11 - 18+00	MAINLINE, LT.	280	-	1,120	-
15+14 - 18+00	MAINLINE, RT.	285	-	1,140	-
22+22 - 25+96	MAINLINE, RT.	376	-	1,504	-
399'B'+63 - 399'B'+96	'B'-LINE (MOUND STREET), LT.	33	-	132	-
400'B'+00 - 401'B'+86	'B'-LINE (MOUND STREET), RT.	187	-	748	-
400'B'+31 - 25+50	'B'-LINE (MOUND STREET), LT.	182	-	728	-
28+00 - 33+50	MAINLINE, RT.	535	-	2,140	-
34+25 - 36+95	MAINLINE, LT.	825	-	3,300	-
37+00 - 40+00	MAINLINE, LT.	860	-	3,440	-
38+00 - 45+00	MAINLINE, RT.	680	-	2,720	-
43+00 - 45+00	MAINLINE, LT.	265	-	1,060	-
46+70 - 49+79	MAINLINE, RT.	314	-	1,256	-
47+00 - 50+64	MAINLINE, LT.	392	-	1,568	-
50+05 - 52+00	MAINLINE, RT.	200	-	800	-
50+86 - 53+78	MAINLINE, LT.	318	-	1,272	-
54+03 - 73+38	MAINLINE, LT.	2,040	-	8,160	-
73+52 - 73+82	MAINLINE, LT.	30	-	120	-
73+98 - 401'E'+88	MAINLINE, LT.	1,344	-	5,376	-
85+63 - 94+72	MAINLINE, RT.	922	-	3,688	-
98+55 - 106+39	MAINLINE, RT.	815	-	3,260	-
99+93 - 104+44	MAINLINE, LT.	450	-	1,800	-
106+57 - 402'F'+20	MAINLINE, RT.	660	-	2,640	-
400'G'+13 - 406'G'+66	'G'-LINE (DRIVEWAY REALIGNMENT), LT.	640	-	2,560	-
115+90 - 124+36	MAINLINE, RT.	865	-	3,460	-
121+38 - 124+39	MAINLINE, LT.	306	-	1,224	-
128+37 - 1'R'+00	MAINLINE, LT.	1,045	-	4,180	-
1'R'+00 - 139+92	MAINLINE, LT.	166	-	664	-
139+23 - 140+30	MAINLINE, RT.	117	-	468	-
140+49 - 146+73	MAINLINE, RT.	628	-	2,512	-
145+00 - 150+96	MAINLINE, LT.	608	-	2,432	-
151+50 - 153+65	MAINLINE, RT.	220	-	880	-
152+50 - 160+50	MAINLINE, LT.	766	-	3,064	-
159+31 - 166+00	MAINLINE, RT.	658	-	2,632	-
164+00 - 165+91	MAINLINE, LT.	198	-	792	-
166+03 - 400'H'+20	MAINLINE, LT.	820	-	3,280	-
403'H'+22 - 171+97	MAINLINE, LT.	134	-	536	-
173+00 - 174+07	MAINLINE, LT.	110	-	440	-
173+00 - 177+75	MAINLINE, RT.	476	-	1,904	-
174+23 - 178+33	MAINLINE, LT.	414	-	1,656	-
179+83 - 184+00	MAINLINE, RT.	418	-	1,672	-
180+00 - 185+00	MAINLINE, LT.	502	-	2,008	-
186+50 - 191+40	MAINLINE, RT.	494	-	1,976	-
187+90 - 195+08	MAINLINE, LT.	738	-	2,952	-
195+29 - 202+69	MAINLINE, LT.	760	-	3,040	-
204+50 - 209+62	MAINLINE, RT.	530	-	2,120	-
209+81 - 212+56	MAINLINE, RT.	335	-	1,340	-
212+74 - 215+71	MAINLINE, RT.	345	-	1,380	-
215+85 - 220+00	MAINLINE, RT.	452	-	1,808	-
224+00 - 232+84	MAINLINE, LT.	200	-	800	-
226+00 - 232+84	MAINLINE, LT.	-	658	-	2,632
234+78 - 402'I'+50	MAINLINE, LT.	-	1,094	-	4,376
402'I'+50 - 405'I'+00	'I'-LINE (EAST PECATONICA ROAD), LT.	-	258	-	1,032
239+14 - 245+67	MAINLINE, LT.	-	654	-	2,616
243+69 - 251+00	MAINLINE, RT.	-	720	-	2,880
254+50 - 257+04	MAINLINE, RT.	-	336	-	1,344
256+50 - 260+00	MAINLINE, LT.	-	400	-	1,600
261+00 - 400'J'+50	MAINLINE, LT.	-	855	-	3,420
	UNDISTRIBUTED	6,417	1,250	25,668	5,000
CATEGORY TOTALS =		32,050	6,225	128,200	24,900
PROJECT TOTALS =		38,275		153,100	

FENCE SAFETY

STATION - STATION	LOCATION	616.0700.S (CAT 020) (LF)
245+00 - 250+00	MAINLINE, LT.	485
PROJECT TOTAL =		485

CULVERT PIPE CHECKS

STATION	LOCATION	628.7555	
		(CAT 010) (EACH)	(CAT 020) (EACH)
405'A'+66	'A'-LINE (STH 78), LT.	3	-
30+44	MAINLINE, LT.	2	-
400'D'+15	'D'-LINE (SHARED USE ACCESS), RT.	2	-
402'D'+15	'D'-LINE (SHARED USE ACCESS), RT.	2	-
402'D'+65	'D'-LINE (SHARED USE ACCESS), RT.	5	-
36+35	MAINLINE, LT.	2	-
42+23	MAINLINE, RT.	19	-
60+70	MAINLINE, RT.	19	-
65+84	MAINLINE, RT.	3	-
75+53	MAINLINE, RT.	3	-
401'E'+83	'E'-LINE (HORSESHOE BEND ROAD), LT.	2	-
86+80	MAINLINE, LT.	3	-
91+62	MAINLINE, LT.	3	-
99+88	MAINLINE, LT.	3	-
108+34	MAINLINE, LT.	3	-
402'G'+32	'G'-LINE (DRIVEWAY REALIGNMENT), RT.	2	-
117+71	MAINLINE, LT.	3	-
132+77	MAINLINE, RT.	3	-
142+72	MAINLINE, LT.	2	-
152+04	MAINLINE, LT.	2	-
154+61	MAINLINE, RT.	3	-
401'H'+24	'H'-LINE (MCKENNA ROAD), LT.	2	-
402'H'+00	'H'-LINE (MCKENNA ROAD), LT.	3	-
188+83	MAINLINE, LT.	3	-
194+25	MAINLINE, RT.	2	-
212+02	MAINLINE, LT.	3	-
212+73	MAINLINE, LT.	19	-
222+00	MAINLINE, RT.	2	-
226+63	MAINLINE, RT.	-	3
236+27	MAINLINE, RT.	-	2
237+71	MAINLINE, RT.	-	3
402'I'+00	'I'-LINE (EAST PECATONICA ROAD), RT.	-	2
405'I'+00	'I'-LINE (EAST PECATONICA ROAD), LT.	-	5
247+70	MAINLINE, LT.	-	3
251+44	MAINLINE, LT.	-	2
256+88	MAINLINE, RT.	-	2
258+76	MAINLINE, RT.	-	2
262+93	MAINLINE, RT.	-	2
264+93	MAINLINE, RT.	-	3
	UNDISTRIBUTED	37	11
CATEGORY TOTALS =		160	40
PROJECT TOTAL =		200	

TEMPORARY STONE DITCH CHECKS

STATION	LOCATION	SPV.0035.01	
		(CAT 010) (CY)	(CAT 020) (CY)
403'A'+50	'A'-LINE (STH 78), LT.	0.6	-
11+00	MAINLINE, LT.	0.6	-
11+00	MAINLINE, RT.	0.6	-
15+50	MAINLINE, LT.	0.6	-
15+50	MAINLINE, RT.	0.6	-
29+50	MAINLINE, RT.	0.6	-
30+75	MAINLINE, LT.	0.6	-
53+30	MAINLINE, RT.	0.6	-
109+00	MAINLINE, LT.	0.6	-
114+00	MAINLINE, LT.	0.6	-
401'B'+25	MAINLINE, LT.	0.6	-
401'B'+25	MAINLINE, RT.	0.6	-
403'H'+38	'H'-LINE (MCKENNA ROAD), LT.	0.6	-
174+50	MAINLINE, LT.	0.6	-
259+25	MAINLINE, RT.	-	0.6
261+50	MAINLINE, LT.	-	0.6
264+60	MAINLINE, RT.	-	0.6
	UNDISTRIBUTED	3.2	0.6
CATEGORY TOTALS =		11.6	2.4
PROJECT TOTAL =		14.0	

CRACK AND DAMAGE SURVEY

NAME	STREET ADDRESS	999.1501.S (CAT 020) (EACH)
ERIC NEAL	1227 COUNTY ROAD F	1
PROJECT TOTAL =		1

SEDIMENTATION BASIN LAYER

STATION	LOCATION	SPV.0180.01 (CAT 010) (SY)
401'A'+25	'A'-LINE (STH 78), LT. (STORMWATER POND)	530
TOTAL =		530

TEMPORARY DITCH CHECKS

STATION	LOCATION	628.7504 (CAT 010) (LF)
405'A+85	A'-LINE (STH 78). LT.	8
31-35	MAINLINE. LT.	8
31-55	MAINLINE. LT.	8
32-00	MAINLINE. LT.	8
32-50	MAINLINE. LT.	8
33-00	MAINLINE. LT.	8
33-30	MAINLINE. LT.	8
33-60	MAINLINE. LT.	8
400'C+05	'C'-LINE (P.E.). RT.	8
400'C+25	'C'-LINE (P.E.). RT.	8
400'C+50	'C'-LINE (P.E.). RT.	8
40'D+50	'D'-LINE (SHARED USE ACCESS). RT.	8
402'D+00	'D'-LINE (SHARED USE ACCESS). RT.	8
34-00	MAINLINE. RT.	8
34-50	MAINLINE. RT. & LT.	16
35-00	MAINLINE. RT. & LT.	16
35-50	MAINLINE. LT.	8
35-70	MAINLINE. RT.	8
36-00	MAINLINE. LT.	8
36-30	MAINLINE. RT.	8
37-00	MAINLINE. RT.	8
38-00	MAINLINE. RT.	8
52-00	MAINLINE. RT.	8
53-00	MAINLINE. RT.	8
-	-	-
61-00	MAINLINE. RT.	8
62-00	MAINLINE. RT.	8
63-50	MAINLINE. RT.	8
69-00	MAINLINE. RT.	8
70-00	MAINLINE. RT.	8
71-00	MAINLINE. RT.	8
71-75	MAINLINE. RT.	8
72-35	MAINLINE. RT.	8
73-00	MAINLINE. RT.	8
73-60	MAINLINE. RT.	8
74-20	MAINLINE. RT.	8
74-75	MAINLINE. RT.	8
76-00	MAINLINE. RT.	8
77-00	MAINLINE. RT.	8
77-50	MAINLINE. RT.	8
78-00	MAINLINE. RT.	8
78-50	MAINLINE. RT.	8
79-00	MAINLINE. RT.	8
79-30	MAINLINE. RT.	8
79-70	MAINLINE. RT.	8
80-00	MAINLINE. RT.	8
80-25	MAINLINE. RT.	8
80-50	MAINLINE. RT.	8
80-75	MAINLINE. RT.	8
81-00	MAINLINE. RT.	8
81-25	MAINLINE. RT.	8
81-50	MAINLINE. RT.	8
81-75	MAINLINE. RT.	8
82-00	MAINLINE. RT.	8
82-25	MAINLINE. RT.	8
82-50	MAINLINE. RT.	8
SUBTOTAL =		456

TEMPORARY DITCH CHECKS

STATION	LOCATION	628.7504 (CAT 010) (LF)
82-75	MAINLINE. RT.	8
83-00	MAINLINE. RT.	8
83-25	MAINLINE. RT.	8
83-50	MAINLINE. RT.	8
83-75	MAINLINE. RT.	8
84-00	MAINLINE. RT.	8
84-25	MAINLINE. RT.	8
84-50	MAINLINE. RT.	8
84-75	MAINLINE. RT.	8
85-00	MAINLINE. RT.	8
402'E+00	'E'-LINE (HORSESHOE BEND ROAD). LT.	8
402'E+50	'E'-LINE (HORSESHOE BEND ROAD). LT.	8
403'E+00	'E'-LINE (HORSESHOE BEND ROAD). LT.	8
87-00	MAINLINE. LT.	8
87-75	MAINLINE. LT.	8
88-50	MAINLINE. LT.	8
89-25	MAINLINE. LT.	8
89-85	MAINLINE. LT.	8
90-50	MAINLINE. LT.	8
91-00	MAINLINE. LT.	8
92-00	MAINLINE. LT.	8
92-50	MAINLINE. LT.	8
93-00	MAINLINE. LT.	8
93-50	MAINLINE. LT.	8
94-00	MAINLINE. LT.	8
94-50	MAINLINE. LT.	8
95-00	MAINLINE. RT. & LT.	16
95-50	MAINLINE. RT. & LT.	16
95-80	MAINLINE. RT.	8
96-00	MAINLINE. RT.	8
96-30	MAINLINE. RT.	8
97-00	MAINLINE. RT.	8
99-00	MAINLINE. LT.	8
99-65	MAINLINE. LT.	8
*05+00	MAINLINE. LT.	8
*06+50	MAINLINE. LT.	8
400'G+15	'G'-LINE (DRIVEWAY REALIGNMENT). RT.	8
400'G+50	'G'-LINE (DRIVEWAY REALIGNMENT). RT.	8
400'G+75	'G'-LINE (DRIVEWAY REALIGNMENT). RT.	8
401'G+00	'G'-LINE (DRIVEWAY REALIGNMENT). RT.	8
401'G+25	'G'-LINE (DRIVEWAY REALIGNMENT). RT.	8
401'G+50	'G'-LINE (DRIVEWAY REALIGNMENT). RT.	8
401'G+75	'G'-LINE (DRIVEWAY REALIGNMENT). RT.	8
402'G+00	'G'-LINE (DRIVEWAY REALIGNMENT). RT.	8
402'G+50	'G'-LINE (DRIVEWAY REALIGNMENT). RT.	8
403'G+00	'G'-LINE (DRIVEWAY REALIGNMENT). RT.	8
403'G+50	'G'-LINE (DRIVEWAY REALIGNMENT). RT.	8
404'G+00	'G'-LINE (DRIVEWAY REALIGNMENT). RT.	8
404'G+50	'G'-LINE (DRIVEWAY REALIGNMENT). RT.	8
405'G+00	'G'-LINE (DRIVEWAY REALIGNMENT). RT.	8
405'G+50	'G'-LINE (DRIVEWAY REALIGNMENT). RT.	8
*15+00	MAINLINE. LT.	8
*16+00	MAINLINE. LT.	8
*17+50	MAINLINE. LT.	8
*18+50	MAINLINE. LT.	8
*24+75	MAINLINE. RT. & LT.	16
*28+00	MAINLINE. RT. & LT.	16
SUBTOTAL =		488

TEMPORARY DITCH CHECKS

STATION	LOCATION	628.7504 (CAT 010) (LF)
128+65	MAINLINE. RT.	8
129+30	MAINLINE. RT.	8
130+00	MAINLINE. RT.	8
130+75	MAINLINE. RT.	8
131+75	MAINLINE. RT.	8
132+50	MAINLINE. RT.	8
133+50	MAINLINE. RT.	8
136+00	MAINLINE. RT.	8
138+00	MAINLINE. RT.	8
140+00	MAINLINE. LT.	8
143+50	MAINLINE. LT.	8
147+00	MAINLINE. RT.	8
148+00	MAINLINE. RT.	8
148+60	MAINLINE. RT.	8
149+35	MAINLINE. RT.	8
150+30	MAINLINE. RT.	8
151+00	MAINLINE. LT.	8
155+00	MAINLINE. RT.	8
156+00	MAINLINE. RT.	8
167+00	MAINLINE. RT.	8
169+00	MAINLINE. RT.	8
170+00	MAINLINE. RT.	8
171+00	MAINLINE. RT.	8
179+75	MAINLINE. LT.	8
186+25	MAINLINE. LT.	8
187+00	MAINLINE. LT.	8
192+00	MAINLINE. RT.	8
192+75	MAINLINE. RT.	8
193+20	MAINLINE. RT.	8
194+50	MAINLINE. RT.	8
195+00	MAINLINE. RT.	8
197+00	MAINLINE. RT.	8
199+00	MAINLINE. RT.	8
221+35	MAINLINE. RT.	8
225+00	MAINLINE. LT.	8
226+00	MAINLINE. LT.	8
227+00	MAINLINE. LT.	8
228+00	MAINLINE. LT.	8
229+00	MAINLINE. LT.	8
229+70	MAINLINE. LT.	8
210+35	MAINLINE. LT.	8
211+00	MAINLINE. LT.	8
215+00	MAINLINE. LT.	8
215+75	MAINLINE. LT.	8
216+25	MAINLINE. LT.	8
216+75	MAINLINE. LT.	8
217+25	MAINLINE. LT.	8
217+75	MAINLINE. LT.	8
218+00	MAINLINE. LT.	8
218+25	MAINLINE. LT.	8
218+50	MAINLINE. LT.	8
218+75	MAINLINE. LT.	8
219+00	MAINLINE. LT.	8
219+35	MAINLINE. LT.	8
219+75	MAINLINE. LT.	8
220+00	MAINLINE. RT.	8
220+25	MAINLINE. RT. & LT.	16
SUBTOTAL =		464

TEMPORARY DITCH CHECKS

STATION	LOCATION	628.7504 (CAT 010) (LF)	628.7504 (CAT 020) (LF)
220+60	MAINLINE. RT.	8	-
220+75	MAINLINE. LT.	8	-
221+00	MAINLINE. RT.	8	-
221+25	MAINLINE. RT. & LT.	16	-
233+00	MAINLINE. RT.	-	8
234+00	MAINLINE. RT. & LT.	-	16
235+00	MAINLINE. RT.	-	8
236+00	MAINLINE. RT.	-	8
237+25	MAINLINE. RT.	-	8
238+00	MAINLINE. RT.	-	8
238+75	MAINLINE. RT.	-	8
239+25	MAINLINE. RT.	-	8
240+00	MAINLINE. RT.	-	8
241+00	MAINLINE. RT.	-	8
242+00	MAINLINE. RT.	-	8
243+00	MAINLINE. RT.	-	8
405'J+20	'J'-LINE (EAST PECATONICA ROAD). LT.	-	8
405'J+50	'J'-LINE (EAST PECATONICA ROAD). LT.	-	8
406'J+00	'J'-LINE (EAST PECATONICA ROAD). LT.	-	8
406'J+25	'J'-LINE (EAST PECATONICA ROAD). LT.	-	8
406'J+50	'J'-LINE (EAST PECATONICA ROAD). LT.	-	8
406'J+75	'J'-LINE (EAST PECATONICA ROAD). LT.	-	8
407'J+00	'J'-LINE (EAST PECATONICA ROAD). LT.	-	8
407'J+25	'J'-LINE (EAST PECATONICA ROAD). LT.	-	8
407'J+50	'J'-LINE (EAST PECATONICA ROAD). LT.	-	8
407'J+75	'J'-LINE (EAST PECATONICA ROAD). LT.	-	8
408'J+00	'J'-LINE (EAST PECATONICA ROAD). LT.	-	8
408'J+25	'J'-LINE (EAST PECATONICA ROAD). LT.	-	8
408'J+75	'J'-LINE (EAST PECATONICA ROAD). LT.	-	8
246+50	MAINLINE. LT.	-	8
248+00	MAINLINE. LT.	-	8
248+75	MAINLINE. LT.	-	8
249+25	MAINLINE. LT.	-	8
250+00	MAINLINE. LT.	-	8
250+25	MAINLINE. LT.	-	8
250+75	MAINLINE. LT.	-	8
251+00	MAINLINE. RT. & LT.	-	16
251+50	MAINLINE. RT.	-	8
251+75	MAINLINE. LT.	-	8
252+00	MAINLINE. RT. & LT.	-	16
252+50	MAINLINE. RT.	-	8
253+00	MAINLINE. RT. & LT.	-	16
252+00	MAINLINE. RT.	-	8
262+75	MAINLINE. RT.	-	8
264+00	MAINLINE. RT.	-	8
264+50	MAINLINE. RT.	-	8
406 J'+50	'J'-LINE (STH 39). RT.	-	8
407 J'+00	'J'-LINE (STH 39). RT.	-	8
UNDISTRIBUTED		52	36
CATEGORY SUBTOTAL =		202	420
CATEGORY TOTALS =		1610	420
PROJECT TOTAL =		2030	
SUBTOTAL =		464	

PERMANENT SIGNING

APPROX. STATION	POSITION	SITE ID	SIGN CODE	SIGN DESCRIPTION	ORDER LINES	SIGN SIZE (INXIN)	634.0614 POSTS WOOD 4X6 INCH 14 FT (CAT 010) (EACH)	634.0616 POSTS WOOD 4X6 INCH 16 FT (CAT 010) (EACH)	637.2210 SIGNS TYPE II REFLECTIVE H (CAT 010) (S.F.)	637.2230 SIGNS TYPE II REFLECTIVE F (CAT 010) (S.F.)	638.2602 REMOVING SIGNS TYPE II (CAT 010) (EACH)	638.3000 REMOVING SMALL SIGN SUPPORTS (CAT 010) (EACH)
INTERSECTION OF STH 78/ EAST BAKER STREET		1-04R	J12-1	M1-5A; M6-1	CTH F; LEFT	---	---	---	---	---	1	1
INTERSECTION OF STH 78/ EAST BAKER STREET		1-05R	---	COUNTY MARKER	IOWA	---	---	---	---	---	1	1
INTERSECTION OF STH 78/ EAST BAKER STREET		1-06R	J12-1	M1-5A; M5-1	CTH F; LEFT	---	---	---	---	---	1	1
INTERSECTION OF STH 78/ EAST BAKER STREET		1-17R	J1-1	M2-1; M1-5A	CTH F	---	---	---	---	---	1	1
INTERSECTION OF STH 78/ EAST BAKER STREET		1-18R	---	COUNTY MARKER	IOWA	---	---	---	---	---	1	1
INTERSECTION OF STH 78/ EAST BAKER STREET		1-19R	J3-2	M3-1, M1-5A, M6-1; M3-1, M1-5A, M6-1	SOUTH, CTH F, STRAIGHT; NORTH, CTH F, RIGHT	---	---	---	---	---	1	1
INTERSECTION OF STH 78/ EAST BAKER STREET		1-20R	---	COUNTY MARKER	IOWA	---	---	---	---	---	1	1
INTERSECTION OF STH 78/ EAST BAKER STREET		1-21R	J12-2	M1-6, M6-4; M1-5A, M6-1	78, LEFT/RIGHT; CTH F, RIGHT	---	---	---	---	---	1	1
398'A+55	STH 78, LT.	1-15	W14-3	NO PASSING ZONE	---	46X36	---	1	---	5.56	---	---
400'A+35	STH 78, RT.	1-07	J12-1	M1-5A; M5-1	CTH F; LEFT	24X45	1	---	7.50	---	---	---
400'A+96	STH 78, LT.	1-14	W3-5	SPEED REDUCTION AHEAD ___ MPH SYMBOL	35	36X36	---	1	---	9.00	---	---
401'A+46	STH 78, LT.	1-13	J4-2	M3-1; M1-6; M3-1; M1-5A	SOUTH, 78; SOUTH, CTH F	48X36	1	---	12.00	---	---	---
402'A+00	STH 78, LT.	1-12R	W14-3	NO PASSING ZONE	---	---	---	---	---	---	1	1
402'A+00	STH 78, LT.	1-11R	W3-5	SPEED REDUCTION AHEAD ___ MPH SYMBOL	---	---	---	---	---	---	1	---
402'A+36	STH 78, RT.	1-03	J12-1	M1-5A; M6-1	CTH F; LEFT	24X45	1	---	7.50	---	---	---
403'A+55	STH 78, LT.	1-09	J3-2	M3-1, M1-5A, M6-1; M3-1, M1-5A, M6-1	SOUTH, CTH F, STRAIGHT; NORTH, CTH F, RIGHT	48X57	---	1	19.00	---	---	---
403'A+70	STH 78, RT.	1-02R	W14-3	NO PASSING ZONE	---	---	---	---	---	---	1	1
405'A+02	STH 78, RT.	1-01	J4-1	M3-1; M1-6	NORTH; 78	24X36	1	---	6.00	---	---	---
406'A+97	STH 78, RT.	1-00	W14-3	NO PASSING ZONE	---	48X36	---	1	---	5.56	---	---
406'A+97	STH 78, LT.	1-08	J1-1	M2-1; M1-5A	---	24X39	1	---	6.50	---	---	---
10+48	MAINLINE, LT.	1-16	J12-2	M1-6, M6-4; M1-5A, M6-1	78, LEFT/RIGHT; CTH F, RIGHT	48X45	---	1	15.00	---	---	---
10+50	MAINLINE, LT.	1-10	R1-1	STOP	---	30X30	1	---	5.18	---	---	---
12+50	MAINLINE, LT.	2-00	W3-1	STOP AHEAD	---	30X30	1	---	---	6.25	---	---
13+00	MAINLINE, RT.	2-01	R2-1	SPEED LIMIT	---	24X30	1	---	5.00	---	---	---
14+00	MAINLINE, LT.	2-02	J2-1	M3-1, M1-5A, M5-1	SOUTH, CTH F, RIGHT	24X57	---	1	9.50	---	---	---
16+00	MAINLINE, LT.	2-03	J1-1	M2-1, M1-5A	78	24X39	---	1	6.50	---	---	---
17+50	MAINLINE, RT.	2-04	W2-2	SIDE ROAD (RIGHT ANGLE) SYMBOL	LEFT	30X30	---	1	---	6.25	---	---
23+50	MAINLINE, LT.	3-09	W7-1	HILL SYMBOL	---	30X30	---	1	---	6.25	---	---
23+50	MAINLINE, LT.	3-10	W7-3	% GRADE	9	24X18	---	---	---	3.00	---	---
24+49	MAINLINE, LT.	3-07R	W1-6	ONE-DIRECTION LARGE ARROW	LEFT	---	---	---	---	---	1	1
24+61	MAINLINE, LT.	3-11R	W1-8	CHEVRON	LEFT	---	---	---	---	---	1	1
24+72	MAINLINE, LT.	3-12R	W1-8	CHEVRON	LEFT	---	---	---	---	---	1	1
24+89	MAINLINE, LT.	3-13R	W1-8	CHEVRON	RIGHT	---	---	---	---	---	1	1
24+95	MAINLINE, LT.	3-14R	W1-6	ONE-DIRECTION LARGE ARROW	RIGHT	---	---	---	---	---	1	1
25+14	MAINLINE, RT.	3-15R	W1-8	CHEVRON	RIGHT	---	---	---	---	---	1	1
401'B+64	MOUND STREET, RT.	3-08	R1-1	STOP	---	30X30	---	1	5.18	---	---	---
399'B+64	MOUND STREET, RT.	3-06	W3-1	STOP AHEAD	---	36X36	---	1	---	9.00	---	---
397'B+80	MOUND STREET, RT.	3-04R	W1-1	ROAD TURNS LEFT	---	---	---	---	---	---	1	1
397'B+80	MOUND STREET, RT.	3-05R	W13-1	___ MPH	20	---	---	---	---	---	1	---
INTERSECTION OF MOUND STREET/ WEST BAKER STREET		3-01R	J12-1	M1-5A; M5-1	CTH F; RIGHT	---	---	---	---	---	1	1
INTERSECTION OF MOUND STREET/ WEST BAKER STREET		3-02R	J12-1	M1-5A; M5-1	CTH F; LEFT	---	---	---	---	---	1	---
26+46	MAINLINE, RT.	3-16R	W1-2R	ROAD CURVES AHEAD	RIGHT	---	---	---	---	---	1	1
26+46	MAINLINE, RT.	3-17R	W13-1	___ MPH	40	---	---	---	---	---	1	---
28+77	MAINLINE, RT.	3-18R	I55-56	ADOPT A HIGHWAY	PEC AREA SCHOOLS EMPLOYEES	---	---	---	---	---	1	1
28+85	MAINLINE, RT.	3-19	I55-56	ADOPT A HIGHWAY	---	30X18	---	1	3.75	---	---	---
28+85	MAINLINE, RT.	3-20	I55-56P	ADOPT A HIGHWAY NAME PLATE	PEC AREA SCHOOLS EMPLOYEES	30X18	---	---	3.75	---	---	---
29+25	MAINLINE, LT.	3-21R	R2-1	SPEED LIMIT	25	---	---	---	---	---	1	1
30+07	MAINLINE, LT.	4-00R	W1-1	ROAD TURNS RIGHT	---	---	---	---	---	---	1	1
30+07	MAINLINE, LT.	4-01R	W13-1	___ MPH	20	---	---	---	---	---	1	---
30+14	MAINLINE, LT.	4-02	W2-2	SIDE ROAD (RIGHT ANGLE) SYMBOL	RIGHT	30X30	---	1	---	6.25	---	---
30+36	MAINLINE, RT.	4-04R	R2-1	SPEED LIMIT	55	---	---	---	---	---	1	1
32+00	MAINLINE, RT.	4-03	R2-1	SPEED LIMIT	55	24X30	---	1	5.00	---	---	---
32+00	MAINLINE, LT.	4-05	R2-1	SPEED LIMIT	25	24X30	---	1	5.00	---	---	---
36+50	MAINLINE, LT.	4-06	W3-5	SPEED REDUCTION AHEAD ___ MPH SYMBOL	25	36X36	---	1	---	9.00	---	---
37+17	MAINLINE, LT.	4-07R	W3-5	SPEED REDUCTION AHEAD ___ MPH SYMBOL	25	---	---	---	---	---	1	1
41+09	MAINLINE, RT.	5-01R	W8-9	LOW SHOULDER	---	---	---	---	---	---	1	1
44+29	MAINLINE, RT.	5-02R	W14-3	NO PASSING ZONE	---	---	---	---	---	---	1	1
CATEGORY SUBTOTALS =							8	16	122.36	66.12	31	26

PERMANENT SIGNING

APPROX. STATION	POSITION	SITE ID	SIGN CODE	SIGN DESCRIPTION	ORDER LINES	SIGN SIZE (INXIN)	634.0614 POSTS WOOD 4X6 INCH 14 FT (CAT 010) (EACH)	634.0616 POSTS WOOD 4X6 INCH 16 FT (CAT 010) (EACH)	637.2210 SIGNS TYPE II REFLECTIVE (CAT 010) (S.F.)	637.2230 SIGNS TYPE II REFLECTIVE F (CAT 010) (S.F.)	638.2602 REMOVING SIGNS TYPE II (CAT 010) (EACH)	638.3000 REMOVING SMALL SIGN SUPPORTS (CAT 010) (EACH)
44+29	MAINLINE RT.	5-03	W14-3	NO PASSING ZONE	--	48X36	--	1	--	5.56	--	--
46+32	MAINLINE LT.	5-04R	W1-2L	ROAD CURVES	LEFT	--	--	--	--	--	1	1
46+32	MAINLINE LT.	5-05R	W13-1	___ MPH	35	--	--	--	--	--	1	1
51+83	MAINLINE LT.	6-00R	W14-3	NO PASSING ZONE	--	--	--	--	--	--	1	1
51+83	MAINLINE LT.	6-01	W14-3	NO PASSING ZONE	--	48X36	--	1	--	5.56	--	--
65+63	MAINLINE RT.	7-00R	W14-3	NO PASSING ZONE	--	--	--	--	--	--	1	1
65+63	MAINLINE RT.	7-01	W14-3	NO PASSING ZONE	--	48X36	--	1	--	5.56	--	--
78+29	MAINLINE LT.	9-00R	W14-3	NO PASSING ZONE	--	--	--	--	--	--	1	1
78+29	MAINLINE LT.	9-01	W14-3	NO PASSING ZONE	--	48X36	--	1	--	5.56	--	--
79+00	MAINLINE RT.	9-02	W2-2	SIDE ROAD (RIGHT ANGLE) SYMBOL	LEFT	30X30	--	1	--	6.25	--	--
85+09	MAINLINE LT.	10-00R	--	COUNTY MARKER	IOWA	--	--	--	--	--	1	1
85+09	MAINLINE LT.	10-01R	M1-5A	CTH ROUTE MARKER	CTH F	--	--	--	--	--	1	--
85+10	MAINLINE LT.	10-02	M1-5A	CTH ROUTE MARKER	CTH F	24X24	--	1	4.00	--	--	--
403'E+00	HORSESHOE BEND ROAD RT.	10-04	R1-1	STOP	--	30X30	--	1	5.18	--	--	--
403'E+01	HORSESHOE BEND ROAD RT.	10-03R	R1-1	STOP	--	--	--	--	--	--	1	1
87+03	MAINLINE RT.	10-05R	--	COUNTY MARKER	IOWA	--	--	--	--	--	1	1
87+03	MAINLINE RT.	10-06R	M1-5A	CTH ROUTE MARKER	CTH F	--	--	--	--	--	1	--
87+10	MAINLINE RT.	10-07	M1-5A	CTH ROUTE MARKER	CTH F	24X24	--	1	4.00	--	--	--
94+00	MAINLINE LT.	11-00	W2-2	SIDE ROAD (RIGHT ANGLE) SYMBOL	RIGHT	30X30	--	1	--	6.25	--	--
98+47	MAINLINE LT.	11-01R	S3-1	SCHOOL BUS STOP AHEAD	--	--	--	--	--	--	1	1
98+47	MAINLINE RT.	11-03R	W1-4L	ROAD CURVES AHEAD	LEFT	--	--	--	--	--	1	1
102+53	MAINLINE LT.	12-00	W14-3	NO PASSING ZONE	--	48X36	--	1	--	5.56	--	--
102+57	MAINLINE LT.	12-01R	W14-3	NO PASSING ZONE	--	--	--	--	--	--	1	1
104+22	MAINLINE RT.	12-02R	W14-3	NO PASSING ZONE	--	--	--	--	--	--	1	1
104+27	MAINLINE RT.	12-03	W14-3	NO PASSING ZONE	--	48X36	--	1	--	5.56	--	--
106+00	MAINLINE RT.	12-04	W1-4L	ROAD CURVES AHEAD	LEFT	30X30	--	1	--	6.25	--	--
106+00	MAINLINE RT.	12-05	W13-1	___ MPH	40	18X18	--	--	--	2.25	--	--
400'F+40	WEST MOSCOW ROAD RT.	13-00R	R1-1	STOP	--	--	--	--	--	--	1	1
400'F+32	WEST MOSCOW ROAD RT.	13-01	R1-1	STOP	--	30X30	--	1	5.18	--	--	--
118+50	MAINLINE LT.	13-02R	S3-1	SCHOOL BUS STOP AHEAD	--	--	--	--	--	--	1	1
127+00	MAINLINE LT.	14-00	W1-4L	ROAD CURVES AHEAD	LEFT	30X30	--	1	--	6.25	--	--
127+00	MAINLINE LT.	14-01	W13-1	___ MPH	40	18X18	--	--	--	2.25	--	--
127+96	MAINLINE LT.	14-02R	W1-4L	ROAD CURVES AHEAD	LEFT	--	--	--	--	--	1	1
134+50	MAINLINE RT.	15-00	W14-3	NO PASSING ZONE	--	48X36	--	1	--	5.56	--	--
134+58	MAINLINE RT.	15-01R	W14-3	NO PASSING ZONE	--	--	--	--	--	--	1	1
141+00	MAINLINE LT.	16-00	W14-3	NO PASSING ZONE	--	48X36	--	1	--	5.56	--	--
141+08	MAINLINE LT.	16-01R	W14-3	NO PASSING ZONE	--	--	--	--	--	--	1	1
147+41	MAINLINE RT.	17-00R	W1-4L	ROAD CURVES AHEAD	LEFT	--	--	--	--	--	1	1
150+28	MAINLINE RT.	17-01	W1-4L	ROAD CURVES AHEAD	LEFT	30X30	--	1	--	6.25	--	--
150+28	MAINLINE RT.	17-02	W13-1	___ MPH	45	18X18	--	--	--	2.25	--	--
404'H+06	McKENNA ROAD RT.	19-01R	R1-1	STOP	--	--	--	--	--	--	1	1
404'H+06	McKENNA ROAD RT.	19-00	R1-1	STOP	--	30X30	--	1	5.18	--	--	--
175+84	MAINLINE LT.	20-00	W1-4L	ROAD CURVES AHEAD	LEFT	30X30	--	1	--	6.25	--	--
175+84	MAINLINE LT.	20-01	W13-1	___ MPH	45	18X18	--	--	--	2.25	--	--
179+65	MAINLINE RT.	20-02	W14-3	NO PASSING ZONE	--	48X36	--	1	--	5.56	--	--
179+65	MAINLINE RT.	20-03R	W14-3	NO PASSING ZONE	--	--	--	--	--	--	1	1
184+75	MAINLINE LT.	21-00R	W1-4L	ROAD CURVES AHEAD	LEFT	--	--	--	--	--	1	1
186+52	MAINLINE LT.	21-01	W14-3	NO PASSING ZONE	--	48X36	--	1	--	5.56	--	--
186+56	MAINLINE LT.	21-02R	W14-3	NO PASSING ZONE	--	--	--	--	--	--	1	1
203+77	MAINLINE RT.	23-00R	W1-4R	ROAD CURVES AHEAD	RIGHT	--	--	--	--	--	1	1
210+93	MAINLINE RT.	24-00	W14-3	NO PASSING ZONE	--	48X36	--	1	--	5.56	--	--
211+00	MAINLINE RT.	24-01R	W14-3	NO PASSING ZONE	--	--	--	--	--	--	1	1
213+65	MAINLINE LT.	24-02	W14-3	NO PASSING ZONE	--	48X36	--	1	--	5.56	--	--
213+70	MAINLINE LT.	24-03R	W14-3	NO PASSING ZONE	--	--	--	--	--	--	1	1
221+00	MAINLINE RT.	23-01	W1-2L	ROAD CURVES AHEAD	LEFT	30X30	--	1	--	6.25	--	--
221+00	MAINLINE RT.	23-02	W13-1	___ MPH	45	18X18	--	--	--	2.25	--	--
SUBTOTALS =							0	24	23.54	121.72	27	25
CATEGORY 010 SUBTOTALS =							8	40	145.90	187.84	58	51

PERMANENT SIGNING

APPROX. STATION	POSITION	SITE ID	SIGN CODE	SIGN DESCRIPTION	ORDER LINES	SIGN SIZE (INXIN)	634.0614 POSTS WOOD 4X6 INCH 14 FT (CAT 020) (EACH)	634.0616 POSTS WOOD 4X6 INCH 16 FT (CAT 020) (EACH)	637.2210 SIGNS TYPE II REFLECTIVE H (CAT 020) (S.F.)	637.2230 SIGNS TYPE II REFLECTIVE F (CAT 020) (S.F.)	638.2602 REMOVING SIGNS TYPE II (CAT 020) (EACH)	638.3000 REMOVING SMALL SIGN SUPPORTS (CAT 020) (EACH)
221+00	MAINLINE, RT.	23-02	W13-1	___ MPH	45	18X18	---	---	---	2.25	---	---
232+05	MAINLINE, LT.	26-00	W1-2R	ROAD CURVES AHEAD	RIGHT	30X30	---	1	---	6.25	---	---
232+05	MAINLINE, LT.	26-01	W13-1	___ MPH	45	18X18	---	---	---	2.25	---	---
237+60	MAINLINE, LT.	27-00R	W1-4R	ROAD CURVES AHEAD	RIGHT	---	---	---	---	---	1	1
409'1"+12	EAST PECATONICA ROAD, RT	27-02	R1-1	STOP	---	30X30	---	1	5.18	---	---	---
409'1"+16	EAST PECATONICA ROAD, RT	27-01R	R1-1	STOP	---	---	---	---	---	---	1	1
239+37	MAINLINE, RT.	27-03R	W1-2R	ROAD CURVES	RIGHT	---	---	---	---	---	1	1
239+37	MAINLINE, RT.	27-04R	W14-3	NO PASSING ZONE	---	---	---	---	---	---	1	1
239+43	MAINLINE, RT.	27-05	W14-3	NO PASSING ZONE	---	48X36	---	1	---	5.56	---	---
242+90	MAINLINE, LT.	27-06R	W14-3	NO PASSING ZONE	---	---	---	---	---	---	1	1
243+00	MAINLINE, LT.	27-07	W14-3	NO PASSING ZONE	---	48X36	---	1	---	5.56	---	---
245+90	MAINLINE, RT.	27-08	W1-4R	ROAD CURVES	RIGHT	30X30	---	1	---	6.25	---	---
245+90	MAINLINE, RT.	27-09	W13-1	___ MPH	45	18X18	---	---	---	2.25	---	---
253+62	MAINLINE, RT.	28-00R	W3-1	STOP AHEAD	---	---	---	---	---	---	1	1
257+50	MAINLINE, RT.	29-00	W3-1	STOP AHEAD	---	30X30	---	1	---	6.25	---	---
258+66	MAINLINE, LT.	29-01R	W8-9	LOW SHOULDER	---	---	---	---	---	---	1	1
258+94	MAINLINE, RT.	29-02R	J1-1	M2-1; M1-6	39	---	---	---	---	---	1	1
259+00	MAINLINE, RT.	29-03	J1-1	M2-1; M1-6	39	24X39	---	1	6.50	---	---	---
262+95	MAINLINE, LT.	29-04R	I55-56	ADOPT A HIGHWAY	PEC AREA SCHOOLS EMPLOYEES	---	---	---	---	---	1	1
263+00	MAINLINE, LT.	29-05	I55-56	ADOPT A HIGHWAY	---	30X18	---	1	3.75	---	---	---
263+00	MAINLINE, LT.	29-06	I55-56P	ADOPT A HIGHWAY NAME PLATE	PEC AREA SCHOOLS EMPLOYEES	30X18	---	---	3.75	---	---	---
264+04	MAINLINE, LT.	30-00	W1-4R	ROAD CURVES AHEAD	RIGHT	30X30	---	1	---	6.25	---	---
264+04	MAINLINE, LT.	30-01	W13-1	___ MPH	45	18X18	---	---	---	2.25	---	---
264+42	MAINLINE, LT.	30-02R	W1-4R	ROAD CURVES AHEAD	RIGHT	---	---	---	---	---	1	1
264+86	MAINLINE, RT.	30-03	R1-1	STOP	---	30X30	---	1	5.18	---	---	---
264+86	MAINLINE, RT.	30-04	J12-1	M1-6; M6-4	39; LEFT/RIGHT	24X45	---	1	7.50	---	---	---
264+96	MAINLINE, RT.	30-05R	J12-1	M1-6; M6-4	39; LEFT/RIGHT	---	---	---	---	---	1	1
264+96	MAINLINE, RT.	30-06R	R1-1	STOP	---	---	---	---	---	---	1	1
400'J'+50	STH 39, RT.	30-07	W1-4R	ROAD CURVES AHEAD	RIGHT	30X30	---	1	---	6.25	---	---
400'J'+50	STH 39, RT.	30-08	W13-1	___ MPH	45	18X18	---	---	---	2.25	---	---
401'J'+83	STH 39, RT.	30-09R	W1-4R	ROAD CURVES AHEAD	RIGHT	---	---	---	---	---	1	1
401'J'+83	STH 39, RT.	30-10R	W13-1	___ MPH	45	---	---	---	---	---	1	---
403'J'+50	STH 39, RT.	30-11	I55-56	ADOPT A HIGHWAY	---	30X18	---	1	3.75	---	---	---
403'J'+50	STH 39, RT.	30-12	I55-56P	ADOPT A HIGHWAY NAME PLATE	UP AND OVER 4-H CLUB OF IOWA COUNTY	30X18	---	---	3.75	---	---	---
403'J'+50	STH 39, RT.	30-13R	I55-56	ADOPT A HIGHWAY	UP AND OVER 4-H CLUB OF IOWA COUNTY	---	---	---	---	---	1	1
404'J'+78	STH 39, RT.	30-14	J12-1	M1-5A; M6-4	CTH F; LEFT/RIGHT	24X45	---	1	7.50	---	---	---
405'J'+31	STH 39, RT.	30-15R	J12-1	M1-5A; M6-4	CTH F; LEFT/RIGHT	---	---	---	---	---	1	1
406'J'+93	STH 39, RT.	30-16R	J4-1	M3-2; M1-6	EAST; 39	---	---	---	---	---	1	1
408'J'+00	STH 39, RT.	30-17	J4-1	M3-2; M1-6	EAST; 39	24X36	---	1	6.00	---	---	---
SUBTOTALS =							0	15	52.86	53.62	17	16
CATEGORY 020 SUBTOTALS =							0	15	52.86	53.62	17	16
PROJECT TOTALS =							8	55	198.76	241.46	75	67

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3

TRAFFIC CONTROL

STATION	643.0300		643.0420		643.0705		643.0900		643.092		643.1050		643.5000	
	DRUMS		BARRICADES TYPE III		WARNING LIGHTS TYPE A		SIGNS		COVERING SIGNS TYPE II		SIGNS PCMS		TRAFFIC CONTROL	
	(CAT 010)	(CAT 020)	(CAT 010)	(CAT 020)	(CAT 010)	(CAT 020)	(CAT 010)	(CAT 020)	(CAT 010)	(CAT 020)	(CAT 010)	(CAT 020)	(CAT 010)	(CAT 020)
	(DAYS)	(DAYS)	(DAYS)	(DAYS)	(DAY)	(DAY)	(DAYS)	(DAYS)	(DAYS)	(DAYS)	(DAYS)	(DAYS)	(EACH)	(EACH)
EXISTING STH 78/CTH F INT	-	-	382	-	764	-	382	-	-	-	-	-	-	-
10+24	-	-	954	-	1,158	-	191	-	-	-	-	-	-	-
265+24.10	-	-	954	-	1,158	-	191	-	-	-	-	-	-	-
'A'-LINE	2,714	-	-	-	-	-	1,167	-	-	-	21	-	-	-
'B'-LINE	-	-	382	-	764	-	573	-	-	-	-	-	-	-
'E'-LINE	-	-	382	-	764	-	573	-	-	-	-	-	-	-
'F'-LINE	-	-	382	-	764	-	573	-	-	-	-	-	-	-
AYEN ROAD	-	-	-	-	-	-	191	-	-	-	-	-	-	-
'H'-LINE	-	-	382	-	764	-	573	-	-	-	-	-	-	-
'I'-LINE	-	-	-	382	-	764	-	573	-	-	-	-	-	-
'J'-LINE	-	3,286	-	-	-	-	1,549	-	-	-	14	-	-	-
EXISTING CTH F (NORTH)	-	-	-	-	-	-	764	-	-	-	-	-	-	-
PROJECT	-	-	-	-	-	-	-	-	11 (1 CYCLE)	-	-	0.85	0.15	-
UNSDISTRIBUTED	250	250	-	-	-	-	-	-	-	-	-	-	-	-
CATEGORY TOTALS=	2,964	3,536	3,818	382	6,136	764	6,727	573	11	21	14	0.85	0.15	-
PROJECT TOTAL =	6,500		4,200		6,900		7,300		11		35		1.00	

MARKING REMOVAL LINE 4-INCH

STATION - STATION	LOCATION	646.9000	
		(CAT 010) (LF)	(CAT 020) (LF)
398'A'+55 - 409'A'+04	A'-LINE, LT.	1,050	-
396'J'+06 - 408'J'+47	J'-LINE, RT.	-	1,250
CATEGORY TOTALS =		1,050	1,250
PROJECT TOTALS =		2,300	

REESTABLISH SECTION CORNER MONUMENT

STATION	LOCATION	SPV.0060.03	
		(CAT 010) (EACH)	(CAT 020) (EACH)
47+15.10	MAINLINE	1	-
73+63.21	MAINLINE	1	-
TOTAL =		2	

CONSTRUCTION STAKING

STATION - STATION	LOCATION	650.4500		650.5000		650.9910		650.9920		SPV.0060.01
		SUBGRADE		BASE		SUPPLEMENTAL CONTROL 5034-00-72		SLOPE STAKES		SEDIMENTATION BASIN
		(CAT 010) (LF)	(CAT 020) (LF)	(CAT 010) (LF)	(CAT 020) (LF)	01. (CAT 010) (LS)	02. (CAT 020) (LS)	(CAT 010) (LF)	(CAT 020) (LF)	(CAT 010) (EACH)
10+24 - 226+00	MAINLINE	21,575	-	21,575	-	-	-	21,575	-	-
226+00 - 265+24.12	MAINLINE	-	3,923	-	3,923	-	-	-	3,923	-
399'A'+55 - 406'A'+98.30	'A'-LINE (STH 78)	743	-	743	-	-	-	743	-	-
401'A'+25	'A'-LINE (STH 78), LT. (SEDIMENTATION BASIN)	-	-	-	-	-	-	-	-	1
399'B'+62.87 - 402'B'+03.52	'B'-LINE (MOUND STREET)	240	-	240	-	-	-	240	-	-
400'C'+00 - 401'C'+68.23	'C'-LINE (P.E.)	168	-	168	-	-	-	168	-	-
400'D'+00 - 402'D'+87.03	'D'-LINE (SHARED USE ACCESS)	287	-	287	-	-	-	287	-	-
401'E'+35 - 403'E'+24.08	'E'-LINE (HORSESHOE BEND ROAD)	189	-	189	-	-	-	189	-	-
400'F'+16 - 402'F'+20	'F'-LINE (WEST MOSCOW ROAD)	204	-	204	-	-	-	204	-	-
400'G'+15 - 406'G'+64.33	'G'-LINE (DRIVEWAY REALIGNMENT)	649	-	649	-	-	-	649	-	-
400'H'+20 - 404'H'+28.79	'H'-LINE (MCKENNA ROAD)	409	-	409	-	-	-	409	-	-
401'I'+70 - 409'I'+24.87	'I'-LINE (EAST PECATONICA ROAD)	-	754	-	754	-	-	-	754	-
400'J'+50 - 407'J'+47.45	'J'-LINE (STH 39)	698	-	698	-	-	-	698	-	-
100'O'+00 - 100'O'+61.86	'O'-LINE (NORTH MAIN STREET)	61	-	61	-	-	-	61	-	-
	PROJECT	-	-	-	-	1	1	-	-	-
CATEGORY TOTALS =		25,223	4,677	25,223	4,677	1	1	25,223	4,677	1
PROJECT TOTALS =		29,900		29,900		2		29,900		1

SAWING

STATION	LOCATION	690.0150		690.0250
		(CAT 010) (LF)	(CAT 020) (LF)	SAWING CONCRETE (CAT 010) (LF)
25+57	MAINLINE, RT.	-	-	5
401'A'+25 - 407'A'+07	'A'-LINE (STH 78)	582	-	-
399'B'+63 - 400'B'+00	'B'-LINE (MOUND STREET)	60	-	-
400'C'+00	'C'-LINE (P.E.)	12	-	-
401'E'+88	'E'-LINE (HORSESHOE BEND ROAD)	20	-	-
402'F'+20	'F'-LINE (WEST MOSCOW ROAD)	20	-	-
400'H'+20	'H'-LINE (MCKENNA ROAD)	20	-	-
402'I'+50	'I'-LINE (EAST PECATONICA ROAD)	-	20	-
400'J'+50 - 407'J'+47.45	'J'-LINE (STH 39)	-	698	-
-	DRIVEWAYS - P.E./F.E./C.E.	59	29	-
CATEGORY TOTALS =		773	747	5
PROJECT TOTALS =		1,520		5

NOTE: QUANTITIES FOR CONSTRUCTION STAKING SUBGRADE, CONSTRUCTION STAKING BASE, AND CONSTRUCTION STAKING SLOPE STAKES REQUIRED FOR 'K', 'L', 'M', 'N', 'P', 'Q', AND 'R'-LINES ARE INCLUDED IN THE MAINLINE CONSTRUCTION STAKING QUANTITIES.

3

3

MARKING LINE

STATION - STATION	LOCATION	DESCRIPTION	646.6464		646.6468		646.6120		648.0100	
			COLD WEATHER MARKING		COLD WEATHER MARKING		STOP LINE EPOXY		LOCATING	
			EPOXY 4-INCH	EPOXY 8-INCH	EPOXY 8-INCH	EPOXY 8-INCH	18-INCH	NO-PASSING ZONE	NO-PASSING ZONE	
			(CAT 010)	(CAT 020)	(CAT 010)	(CAT 020)	(CAT 010)	(CAT 020)	(CAT 010)	(CAT 020)
			(LF)	(LF)	(LF)	(LF)	(LF)	(LF)	(MI)	(MI)
398'A'+55 - 409'A'+04.30	'A'-LINE (STH 78)	DOUBLE YELLOW	2,100	-	-	-	-	-	-	-
398'A'+55 - 409'A'+04.31	'A'-LINE (STH 78)	WHITE EDGELINE	1,970	-	-	-	-	-	-	-
403'A'+56 - 405'A'+56	'A'-LINE (STH 78), LT.	LANE EDGE	-	-	200	-	-	-	-	-
10+65 - 226+00	MAINLINE	WHITE EDGELINE	42,570	-	-	-	-	-	-	-
226+00 - 264+60	MAINLINE	WHITE EDGELINE	-	7,540	-	-	-	-	-	-
10+28	MAINLINE	STOP LINE	-	-	-	-	32	-	-	-
10+28 - 44+29	MAINLINE	DOUBLE YELLOW	6,810	-	-	-	-	-	-	-
399'B'+63 - 402'B'+05	'B'-LINE (MOUND STREET)	WHITE EDGELINE	340	-	-	-	-	-	-	-
400'B'+00 - 401'B'+92	'B'-LINE (MOUND STREET)	DOUBLE YELLOW	390	-	-	-	-	-	-	-
401'B'+92	'B'-LINE (MOUND STREET)	STOP LINE	-	-	-	-	26	-	-	-
44+29 - 51+83	MAINLINE	YELLOW PASSING	190	-	-	-	-	-	-	-
51+83 - 57+44	MAINLINE	SB PASSING ONLY	700	-	-	-	-	-	-	-
57+44 - 60+16	MAINLINE	YELLOW PASSING	70	-	-	-	-	-	-	-
60+16 - 65+63	MAINLINE	NB PASSING ONLY	690	-	-	-	-	-	-	-
65+63 - 78+29	MAINLINE	YELLOW PASSING	320	-	-	-	-	-	-	-
78+29 - 86+03	MAINLINE	SB PASSING ONLY	970	-	-	-	-	-	-	-
86+03 - 97+29	MAINLINE	DOUBLE YELLOW	2,260	-	-	-	-	-	-	-
97+29 - 102+57	MAINLINE	NB PASSING ONLY	660	-	-	-	-	-	-	-
102+57 - 104+22	MAINLINE	DOUBLE YELLOW	330	-	-	-	-	-	-	-
104+22 - 110+00	MAINLINE	SB PASSING ONLY	730	-	-	-	-	-	-	-
110+00 - 126+00	MAINLINE	DOUBLE YELLOW	3,200	-	-	-	-	-	-	-
126+00 - 134+58	MAINLINE	NB PASSING ONLY	1,070	-	-	-	-	-	-	-
134+58 - 140+45	MAINLINE	YELLOW PASSING	150	-	-	-	-	-	-	-
140+45 - 149+91	MAINLINE	SB PASSING ONLY	1,190	-	-	-	-	-	-	-
149+91 - 171+66	MAINLINE	DOUBLE YELLOW	4,350	-	-	-	-	-	-	-
171+65 - 179+65	MAINLINE	NB PASSING ONLY	1,000	-	-	-	-	-	-	-
179+65 - 186+55	MAINLINE	YELLOW PASSING	180	-	-	-	-	-	-	-
186+55 - 194+87	MAINLINE	SB PASSING ONLY	1,040	-	-	-	-	-	-	-
194+87 - 202+68	MAINLINE	DOUBLE YELLOW	1,580	-	-	-	-	-	-	-
202+68 - 211+00	MAINLINE	NB PASSING ONLY	1,040	-	-	-	-	-	-	-
211+00 - 213+70	MAINLINE	YELLOW PASSING	80	-	-	-	-	-	-	-
213+70 - 221+56	MAINLINE	SB PASSING ONLY	990	-	-	-	-	-	-	-
221+56 - 226+00	MAINLINE	DOUBLE YELLOW	890	-	-	-	-	-	-	-
226+00 - 229+05	MAINLINE	DOUBLE YELLOW	-	610	-	-	-	-	-	-
229+05 - 239+38	MAINLINE	NB PASSING ONLY	-	1,290	-	-	-	-	-	-
239+38 - 242+91	MAINLINE	YELLOW PASSING	-	90	-	-	-	-	-	-
242+91 - 250+00	MAINLINE	SB PASSING ONLY	-	890	-	-	-	-	-	-
250+00 - 265+17	MAINLINE	DOUBLE YELLOW	-	3,010	-	-	-	-	-	-
265+17	MAINLINE	STOP LINE	-	-	-	-	32	-	-	-
396'J'+06 - 408'J'+48	'J'-LINE (STH 39)	DOUBLE YELLOW	-	2,490	-	-	-	-	-	-
396'J'+06 - 408'J'+48	'J'-LINE (STH 39)	WHITE EDGELINE	-	2,170	-	-	-	-	-	-
402'J'+00 - 405'J'+00	'J'-LINE (STH 39)	LANE EDGE	-	-	-	300	-	-	-	-
10+24 - 226+00	MAINLINE	-	-	-	-	-	-	4.09	-	-
226+00 - 265+24.10	MAINLINE	-	-	-	-	-	-	-	0.74	-
CATEGORY TOTALS =			77,860	18,090	200	300	58	32	4.09	0.74
PROJECT TOTALS =			95,950		500		90		4.83	

PROJECT NO: 5034-00-72

HWY: CTH F

COUNTY: IOWA

MISCELLANEOUS QUANTITIES

SHEET

E

FILE NAME: S:\PROJECTS\K19250 CTH F IOWA COUNTY\SHETSPLAN\DETAILS\K19250_MISCELLANEOUS QUANTITIES.DWG

PLOT DATE: 10/21/2021 2:54:07 PM

PLOT BY: JONAH DRAKE

PLOT SCALE: 1" = 1'

LAYOUT: LAYOUT 10

CONVENTIONAL ABBREVIATIONS

ACCESS POINT/ DRIVEWAY CONNECTION	AP	PROPERTY LINE	PL
ACCESS RIGHTS	AR	RECORDED AS	(100')
ACRES	AC.	REFERENCE LINE	R/L
AND OTHERS	ET.AL.	RELEASE OF RIGHTS	ROR
BARN	B.	REMAINING	REM.
CENTERLINE	C/L	RIGHT-OF-WAY	R/W
CERTIFIED SURVEY MAP	CSM	SECTION	SEC.
CORNER	COR.	SHED	S.
CONVEYANCE OF RIGHTS	CR	STATION	STA.
DOCUMENT	DOC.	TEMPORARY LIMITED EASEMENT	TLE
EASEMENT	EASE.	VOLUME	V.
GARAGE	G.	CURVE DATA	
HIGHWAY EASEMENT	H.E.	LONG CHORD	LCH
HOUSE	H.	LONG CHORD BEARING	LCB
HOUSE TRAILER	H.T.	RADIUS	R
LAND CONTRACT	LC	DEGREE OF CURVE	D
MONUMENT	MON.	CENTRAL ANGLE OR DELTA	DELTA
PAGE	P.	LENGTH OF CURVE	L
PERMANENT LIMITED EASEMENT	PLE	TANGENT	TAN

CONVENTIONAL SYMBOLS

FOUND SURVEY MONUMENT (WITH POINT NUMBER)	○ 1040	PROPOSED R/W LINE	---
R/W MONUMENT	○ ● (SET)	EXISTING H.E. LINE	---
R/W STANDARD	△ ▲ (SET)	PROPERTY LINE	---
SIGN	ISIGN	LOT & TIE LINES	---
SECTION CORNER MONUMENT	⊕	SLOPE INTERCEPTS	///
SECTION CORNER SYMBOL	⊕	CORPORATE LIMITS	///
FEE (HATCH VARIES)	///	NO ACCESS (BY PREVIOUS ACQUISITION/CONTROL)	---
TEMPORARY LIMITED EASEMENT	---	NO ACCESS (BY ACQUISITION)	---
PERMANENT LIMITED EASEMENT	---	NO ACCESS (BY STATUTORY AUTHORITY)	---
R/W BOUNDARY POINT	⊕	SECTION LINE	---
PARCEL NUMBER	Ⓚ	QUARTER LINE	---
UTILITY PARCEL NUMBER	Ⓚ	SIXTEENTH LINE	---
SIGN NUMBER (OFF PREMISE)	Ⓚ	EXISTING CENTERLINE	---
BUILDING	Ⓚ	PROPOSED REFERENCE LINE	---
		PARALLEL OFFSET	---
		ENCROACHMENT	---
		HIGHWAY EASEMENT	---

CONVENTIONAL UTILITY SYMBOLS

WATER	—W—	SANITARY SEWER	—SAN—
GAS	—G—	STORM SEWER	—SS—
TELEPHONE	—T—		
OVERHEAD TRANSMISSION LINES	—OH—	NON COMPENSABLE	⊕
ELECTRIC	—E—	COMPENSABLE	⊕
CABLE TELEVISION	—TV—	POWER POLE	⊕
FIBER OPTIC	—FO—	TELEPHONE POLE	⊕
		TELEPHONE PEDESTAL	⊕
		ELECTRIC TOWER	⊕

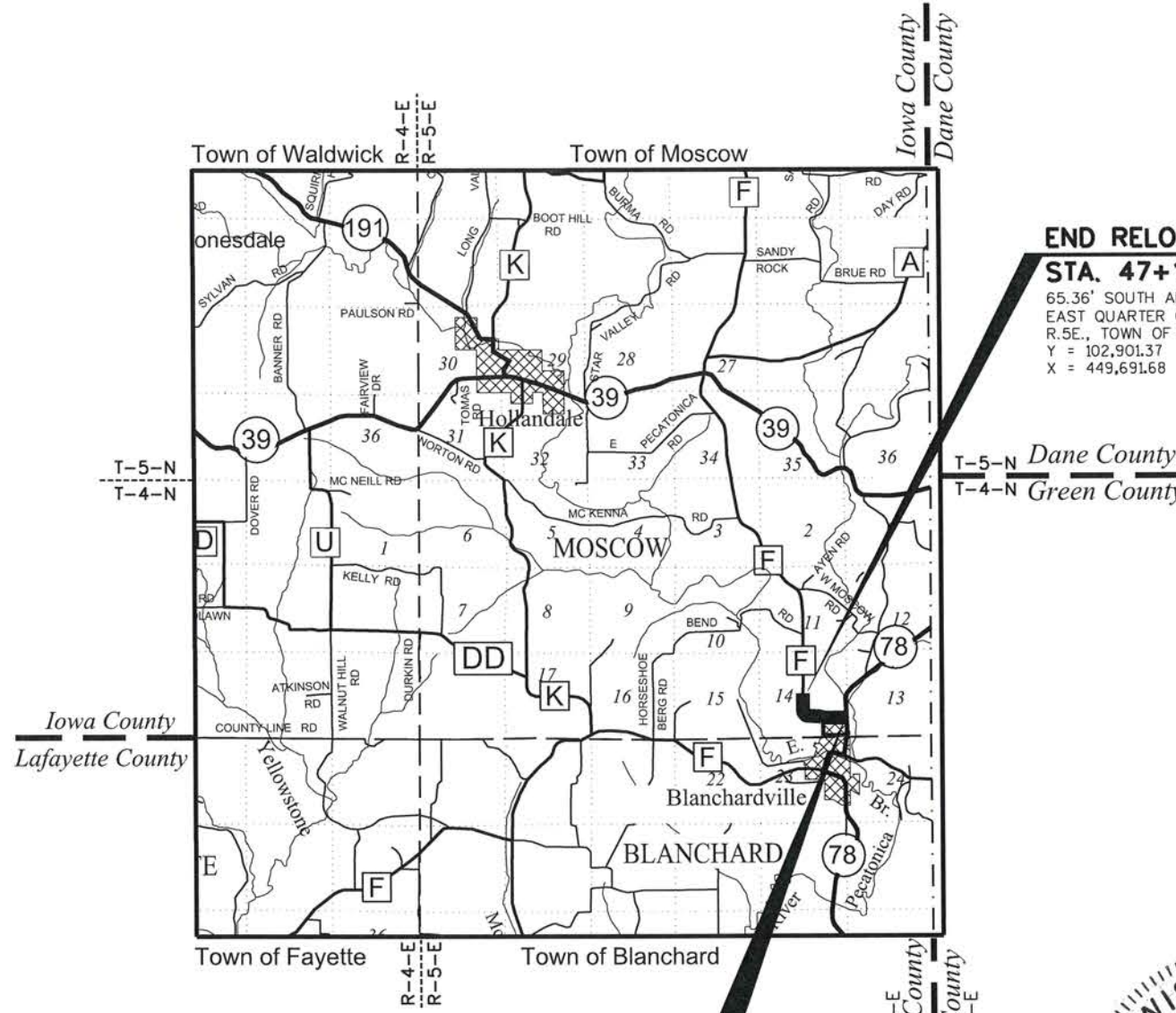
NOTES

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COUNTY COORDINATES, IOWA COUNTY, NAD 83 (2011) IN US SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

RIGHT-OF-WAY MONUMENTS ARE TYPE 2 MONUMENTS (TYPICALLY 3/4" X 24" REBAR) AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS OF PUBLIC RECORD."

R/W PROJECT NUMBER	5034-00-02	SHEET NUMBER	4.01	TOTAL SHEETS	4
FEDERAL PROJECT NUMBER					
PLAT OF RIGHT-OF-WAY REQUIRED FOR BLANCHARDVILLE - DANE COUNTY LINE (STH 78 TO STH 39)					
CTH F			IOWA COUNTY		
CONSTRUCTION PROJECT NUMBER					



END RELOCATION ORDER

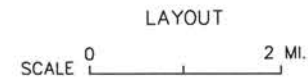
STA. 47+15.10

65.36' SOUTH AND 2624.34' WEST OF THE EAST QUARTER CORNER OF SECTION 14, T.4N., R.5E., TOWN OF MOSCOW, IOWA COUNTY, WI
Y = 102,901.37
X = 449,691.68

BEGIN RELOCATION ORDER

STA. 10+24

1093.35' SOUTH AND 69.82' EAST OF THE EAST QUARTER CORNER OF SECTION 14, T.4N., R.5E., TOWN OF MOSCOW, IOWA COUNTY, WI
Y = 101,873.38
X = 452,385.85



TOTAL NET LENGTH OF CENTERLINE = 0.699 MI.



REVISION DATE
12-18-2020
08-13-2021 N.C

APPROVED FOR IOWA COUNTY
DATE: *[Signature]*
(NAME/TITLE)
[Signature]

SCHEDULE OF LANDS & INTERESTS REQUIRED

PARCEL NUMBER	OWNER (S)	INTEREST REQUIRED	R/W ACRES REQUIRED			TLE ACRES REQ.
			NEW	EXISTING	TOTAL	
1	TERRY D. ERICKSON & KAREN J. ERICKSON, HUSBAND AND WIFE, AS SURVIVORSHIP MARITAL PROPERTY	FEE, T.L.E.	4.53	0.02	4.55	0.22
2	PAULA J. SCHUETTE, BRENDA J. KITE, AND SARA J. ERICKSON, TO EACH A ONE-THIRD INTEREST AS TENANTS IN COMMON	FEE	0.03	-	0.03	---
201	AMERICAN TRANSMISSION COMPANY, LLC	RELEASE OF RIGHTS, TEMP. CONSTR. EASE.				

NOTE: AREAS SHOWN IN THE TOTAL ACRES COLUMN MAY BE APPROXIMATE AND ARE DERIVED FROM THE TAX ROLLS OR OTHER AVAILABLE SOURCES AND MAY NOT INCLUDE LANDS OF THE OWNER WHICH ARE NOT CONTIGUOUS TO THE AREA TO BE ACQUIRED. OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO IOWA COUNTY.

COORDINATE TABLE - FOUND SURVEY MONUMENTS

PT.#	STATION	OFFSET	Y	X	DESCRIPTION
4714	10+57.12	276.35 LT.	101596.05	452362.33	FOUND 3/4" φ IRON ROD

TOWN OF MOSCOW

COORDINATE TABLE - TLE POINTS

PT.#	STATION	OFFSET	Y	X
200	401'B'+50.00	38.05 RT.	101525.12	451031.19
201	401'B'+25.00	60.17 RT.	101500.40	451053.63
202	401'B'+20.00	49.00 RT.	101495.26	451042.52
203	400'B'+85.00	49.00 RT.	101460.26	451042.96
204	400'B'+80.00	42.00 RT.	101455.17	451036.02
230	23+45.00	40.00 LT.	101556.75	451106.77
231	23+45.00	103.82 LT.	101493.10	451111.43
232	23+01.61	107.61 LT.	101494.16	451160.46
233	23+02.35	117.00 LT.	101484.74	451160.78
234	22+80.00	116.00 LT.	101489.23	451185.88
235	22+70.00	34.21 LT.	101571.71	451183.86
236	400'B'+00.00	38.00 RT.	101375.13	451033.03
240	17+90.00	61.47 LT.	101688.29	451655.98
241	17+90.00	83.00 LT.	101667.85	451662.75
242	18+60.00	83.00 LT.	101645.82	451596.31
18+60.00		61.36 LT.	101666.36	451589.50

E1/4 CORNER SEC. 14
FOUND 1 1/4" IRON REBAR
Y = 102,966.73'
X = 452,316.03'

TWO ROD (33') WIDE
UTELCO COMMUNICATION
LINE EASEMENT
DOC. 234147

E 1/4 COR. SEC. 14 TO PT. 100
S01°40'30"E, 976.16'

NE 1/4 - SE 1/4
SEC. 14, T4N, R5E

NW 1/4 - SW 1/4
SEC. 13,
T4N, R5E

**BEGIN RELOCATION ORDER
STA. 10+24**

1093.35' SOUTH AND 69.82' EAST OF THE EAST QUARTER CORNER OF SECTION 14, T.4N., R.5E., TOWN OF MOSCOW, IOWA COUNTY, WI
Y = 101,873.38
X = 452,385.85

RIGHT OF WAY LINE TABLE

POINT TO POINT	BEARING	DISTANCE
100 TO 101	S43°05'46"W	83.06'
101 TO 102	S86°59'18"W	307.61'
102 TO 103	S70°44'09"W	786.66'
103 TO 104	N89°57'28"W	96.23'
104 TO 105	S86°09'35"W	93.46'
105 TO 106	S78°24'56"W	33.81'
106 TO 107	S00°25'07"E	266.72'
107 TO 108	N89°16'52"E	1.89'
108 TO 109	N00°58'31"W	42.04'
109 TO 110	N89°18'03"E	16.00'
110 TO 111	N00°56'53"W	1.28'
112 TO 113	N22°24'58"E	66.48'
113 TO 114	N83°45'11"E	319.38'
114 TO 115	N71°44'33"E	690.25'
115 TO 116	N83°42'45"E	135.20'
116 TO 117	S01°59'14"E	279.00'
117 TO 118	S89°59'04"E	210.13'
118 TO 4714	N09°38'01"W	53.47'
4714 TO 148	N02°36'30"W	382.20'
148 TO 100	N01°38'39"W	13.13'

COORDINATE TABLE - R/W POINTS

PT.#	STATION	OFFSET	Y	X
100	10+61.18	118.96 RT.	101990.98	452344.56
101	11+20.00	60.32 RT.	101930.33	452287.81
102	14+20.00	62.00 RT.	101914.18	451980.63
103	22+00.00	36.00 RT.	101654.63	451238.01
104	23+00.00	54.00 RT.	101654.70	451141.78
105	24+00.00	54.00 RT.	101648.44	451048.53
106	24+33.26	47.38 RT.	101641.65	451015.40
107	400'B'+00.00	22.32 RT.	101374.93	451017.35
108	400'B'+00.00	24.22 RT.	101374.96	451019.25
109	400'B'+42.04	24.03 RT.	101416.99	451018.53
110	400'B'+42.03	40.03 RT.	101417.19	451034.53
111	400'B'+43.31	40.03 RT.	101418.46	451034.51
112	401'B'+20.00	25.23 RT.	101494.96	451018.75
113	24+05.00	38.00 LT.	101556.42	451044.10
114	21+00.00	61.00 LT.	101591.17	451361.58
115	14+00.00	49.00 LT.	101807.42	452017.09
116	12+60.00	43.00 LT.	101822.22	452151.48
117	12+60.00	322.00 LT.	101543.39	452161.15
118	10+50.00	329.34 LT.	101543.33	452371.28
148	10+61.26	105.83 RT.	101977.85	452344.94

C/L CURVE 2
PI STA. = 22+71.45
Y = 101,595.11
X = 451,179.65
R = 833.00
D = 6°52'42"
DELTA = 18°03'05"
L = 262.44
T = 132.32
C = 261.36
PC STA. = 21+39.13
Y = 101,636.75
X = 451,305.24
PT STA. = 24+01.58
Y = 101,594.43
X = 451,047.33

C/L CURVE 1
PI STA. = 14+34.09
Y = 101,859.16
X = 451,976.00
R = 833.00
D = 6°52'42"
DELTA = 16°21'26"
L = 237.81
T = 119.72
C = 237.00
PC STA. = 13+14.37
Y = 101,863.31
X = 452,095.65
PT STA. = 15+52.18
Y = 101,821.48
X = 451,862.36

EASEMENT TABLE

OWNER	RECORDING INFORMATION	LOCATED IN R/W PARCEL #	REMARKS
TDS TELECOM, INC.	DOC. #227001	1	16.5' (1 ROD) WIDE TELEPHONE EASEMENT
UTELCO	DOC. #234147	1	33' (2 ROD) WIDE ELECTRIC LINE EASEMENT
AMERICAN TRANSMISSION COMPANY, LLC	DOC. #74591	1	60' WIDE ELECTRIC LINE EASEMENT

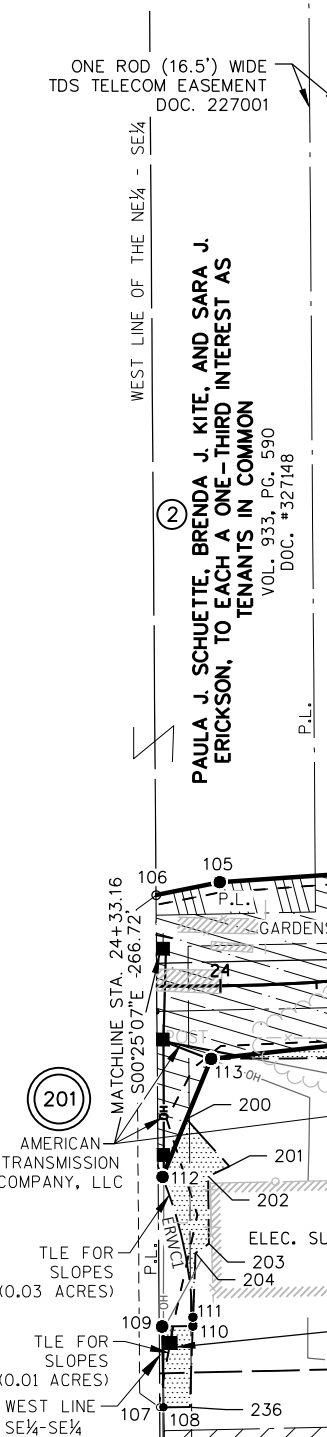
EXISTING RIGHT OF WAY CURVE TABLE (ERWC)

CURVE	POINT TO POINT	DELTA ANGLE	TANGENT	RADIUS	ARC LENGTH	CHORD LENGTH	CHORD BEARING
ERWC1	111 TO 112	21°22'57"	39.74	210.50	78.56	78.10	N11°38'22"W

SE 1/4 - SE 1/4
SEC. 14, T4N, R5E

NOTE: EXISTING C/L OF COUNTY HIGHWAY "F" WAS BASED ON CENTERLINE OF EXISTING PAVEMENT.
BASIS OF EXISTING RIGHT-OF-WAY FOR COUNTY HIGHWAY "F" WAS BASED ON COUNTY RECORDS, THE CENTERLINE OF EXISTING PAVEMENT, AND WIS. STATUTE 82.31(2). BASIS FOR STH 78 TYP 5590-04-21-4.01 AMENDMENT NO. 1.

SE CORNER SEC. 14
FOUND ALUMINUM CAP
MONUMENT
Y = 100,302.57'
X = 452,336.67'



REVISION DATE 12-18-2020 08-13-2021	DATE: SEPTEMBER 11, 2019	SCALE: FEET 0 50 100	HWY: CTH F	STATE R/W PROJECT NUMBER: 5034-00-02	PLAT SHEET 4.02
GRID FACTOR N/A			COUNTY: IOWA	CONSTRUCTION PROJECT NUMBER:	PS&E SHEET E

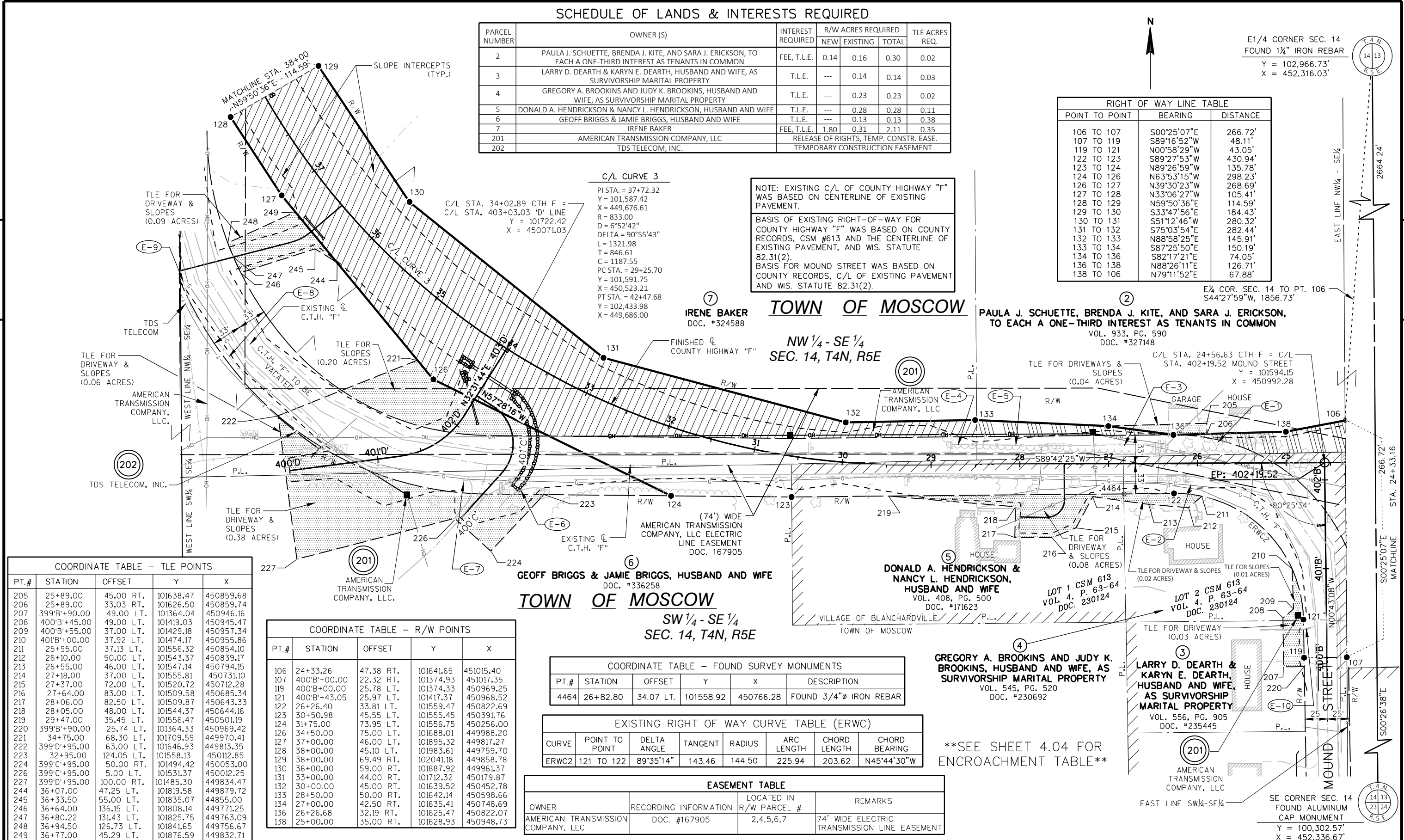
SCHEDULE OF LANDS & INTERESTS REQUIRED

PARCEL NUMBER	OWNER (S)	INTEREST REQUIRED	R/W ACRES REQUIRED			TLE ACRES REQ.
			NEW	EXISTING	TOTAL	
2	PAULA J. SCHUETTE, BRENDA J. KITE, AND SARA J. ERICKSON, TO EACH A ONE-THIRD INTEREST AS TENANTS IN COMMON	FEE, T.L.E.	0.14	0.16	0.30	0.02
3	LARRY D. DEARTH & KARYN E. DEARTH, HUSBAND AND WIFE, AS SURVIVORSHIP MARITAL PROPERTY	T.L.E.	---	0.14	0.14	0.03
4	GREGORY A. BROOKINS AND JUDY K. BROOKINS, HUSBAND AND WIFE, AS SURVIVORSHIP MARITAL PROPERTY	T.L.E.	---	0.23	0.23	0.02
5	DONALD A. HENDRICKSON & NANCY L. HENDRICKSON, HUSBAND AND WIFE	T.L.E.	---	0.28	0.28	0.11
6	GEOFF BRIGGS & JAMIE BRIGGS, HUSBAND AND WIFE	T.L.E.	---	0.13	0.13	0.38
7	IRENE BAKER	FEE, T.L.E.	1.80	0.31	2.11	0.35
201	AMERICAN TRANSMISSION COMPANY, LLC	RELEASE OF RIGHTS, TEMP. CONSTR. EASE.				
202	TDS TELECOM, INC.	TEMPORARY CONSTRUCTION EASEMENT				

RIGHT OF WAY LINE TABLE		
POINT TO POINT	BEARING	DISTANCE
106 TO 107	S00°25'07"E	266.72'
107 TO 119	S89°16'52"W	48.11'
119 TO 121	N00°58'29"W	43.05'
122 TO 123	S89°27'53"W	430.94'
123 TO 124	N89°26'59"W	135.78'
124 TO 126	N63°53'15"W	298.23'
126 TO 127	N39°30'23"W	268.69'
127 TO 128	N33°06'27"W	105.41'
128 TO 129	N59°50'36"E	114.59'
129 TO 130	S33°47'56"E	184.43'
130 TO 131	S51°12'46"W	280.32'
131 TO 132	S75°03'54"E	282.44'
132 TO 133	N88°58'25"E	145.91'
133 TO 134	S87°25'50"E	150.19'
134 TO 136	S82°17'21"E	74.05'
136 TO 138	N88°26'11"E	126.71'
138 TO 106	N79°11'52"E	67.88'

NOTE: EXISTING C/L OF COUNTY HIGHWAY "F" WAS BASED ON CENTERLINE OF EXISTING PAVEMENT.
 BASIS OF EXISTING RIGHT-OF-WAY FOR COUNTY HIGHWAY "F" WAS BASED ON COUNTY RECORDS, CSM #613 AND THE CENTERLINE OF EXISTING PAVEMENT, AND WIS. STATUTE 82.31(2).
 BASIS FOR MOUND STREET WAS BASED ON COUNTY RECORDS, C/L OF EXISTING PAVEMENT AND WIS. STATUTE 82.31(2).

C/L CURVE 3
 PISTA = 37+72.32
 Y = 101,587.42
 X = 449,676.61
 R = 833.00
 D = 6°52'42"
 DELTA = 90°55'43"
 L = 1321.98
 T = 846.61
 C = 1187.55
 PC STA. = 29+25.70
 Y = 101,591.75
 X = 450,523.21
 PT STA. = 42+47.68
 Y = 102,433.98
 X = 449,686.00



COORDINATE TABLE - TLE POINTS				
PT.#	STATION	OFFSET	Y	X
205	25+89.00	45.00 RT.	101638.47	450859.68
206	25+89.00	33.03 RT.	101626.50	450859.74
207	399'B+90.00	49.00 LT.	101364.04	450946.16
208	400'B+45.00	49.00 LT.	101419.03	450945.47
209	400'B+55.00	37.00 LT.	101429.18	450957.34
210	401'B+00.00	37.92 LT.	101474.17	450955.86
211	25+95.00	37.13 LT.	101556.32	450854.10
212	26+10.00	50.00 LT.	101543.37	450839.17
213	26+55.00	46.00 LT.	101547.14	450794.15
214	27+18.00	37.00 LT.	101555.81	450731.10
215	27+37.00	72.00 LT.	101520.72	450712.28
216	27+64.00	83.00 LT.	101509.58	450685.34
217	28+06.00	82.50 LT.	101509.87	450643.33
218	28+05.00	48.00 LT.	101544.37	450644.16
219	29+47.00	35.45 LT.	101556.47	450501.19
220	399'B+90.00	25.74 LT.	101364.33	450969.42
221	34+75.00	68.30 LT.	101709.59	449970.41
222	399'D+95.00	63.00 LT.	101646.93	449813.35
223	32+95.00	124.05 LT.	101558.13	450112.85
224	399'C+95.00	50.00 RT.	101494.42	450053.00
226	399'C+95.00	5.00 LT.	101531.37	450012.25
227	399'D+95.00	100.00 RT.	101485.30	449834.47
244	36+07.00	47.25 LT.	101819.58	449879.72
245	36+33.50	55.00 LT.	101835.07	44855.00
246	36+64.00	136.15 LT.	101808.14	449771.25
247	36+80.22	131.43 LT.	101825.75	449763.09
248	36+94.50	126.73 LT.	101841.65	449756.67
249	36+77.00	45.29 LT.	101876.59	449832.71

COORDINATE TABLE - R/W POINTS				
PT.#	STATION	OFFSET	Y	X
106	24+33.26	47.38 RT.	101641.65	451015.40
107	400'B+00.00	22.32 RT.	101374.93	451017.35
119	400'B+00.00	25.78 LT.	101374.33	450969.25
121	400'B+43.05	25.97 LT.	101417.37	450968.52
122	26+26.40	33.81 LT.	101559.47	450822.69
123	30+50.98	45.55 LT.	101555.45	450391.76
124	31+75.00	73.95 LT.	101556.75	450256.00
126	34+50.00	75.00 LT.	101688.01	449988.20
127	37+00.00	46.00 LT.	101895.32	449817.27
128	38+00.00	45.10 LT.	101983.61	449759.70
129	38+00.00	69.49 RT.	102041.18	449858.78
130	36+00.00	59.00 RT.	101887.92	449961.37
131	33+00.00	44.00 RT.	101712.32	450179.87
132	30+00.00	45.00 RT.	101639.52	450452.78
133	28+50.00	50.00 RT.	101642.14	450598.66
134	27+00.00	42.50 RT.	101635.41	450748.69
136	26+26.68	32.19 RT.	101625.47	450822.07
138	25+00.00	35.00 RT.	101628.93	450948.73

COORDINATE TABLE - FOUND SURVEY MONUMENTS					
PT.#	STATION	OFFSET	Y	X	DESCRIPTION
4464	26+82.80	34.07 LT.	101558.92	450766.28	FOUND 3/4" Ø IRON REBAR

EXISTING RIGHT OF WAY CURVE TABLE (ERWC)							
CURVE	POINT TO POINT	DELTA ANGLE	TANGENT	RADIUS	ARC LENGTH	CHORD LENGTH	CHORD BEARING
ERWC2	121 TO 122	89°35'14"	143.46	144.50	225.94	203.62	N45°44'30"W

EASEMENT TABLE			
OWNER	RECORDING INFORMATION	LOCATED IN R/W PARCEL #	REMARKS
AMERICAN TRANSMISSION COMPANY, LLC	DOC. #167905	2,4,5,6,7	74' WIDE ELECTRIC TRANSMISSION LINE EASEMENT

SEE SHEET 4.04 FOR ENCROACHMENT TABLE

REVISION DATE 12-18-2020 08-13-2021	DATE: SEPTEMBER 11, 2019	SCALE: FEET 0 50 100	HWY: CTH F COUNTY: IOWA	STATE R/W PROJECT NUMBER: 5034-00-02 CONSTRUCTION PROJECT NUMBER:	PLAT SHEET 4.03 PS&E SHEET E
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COORDINATE TABLE - TLE POINTS				
PT.#	STATION	OFFSET	Y	X
237	43+60.00	92.00 L	102547.32	449595.26
238	43+90.00	92.00 L	102577.32	449595.59
239	44+05.00	55.00 L	102591.91	449632.75

COORDINATE TABLE - R/W POINTS				
PT.#	STATION	OFFSET	Y	X
128	38+00.00	45.100 L.T.	101983.61	449759.70
129	38+00.00	69.49 RT.	102041.18	449858.78
139	39+00.00	57.00 L.T.	102073.01	449701.40
140	40+00.00	53.00 L.T.	102174.58	449669.00
149	42+05.00	55.00 L.T.	102389.11	449631.67
150	42+05.00	75.00 L.T.	102388.30	449611.68
151	42+35.00	75.00 L.T.	102421.00	449610.96
141	42+35.00	55.00 L.T.	102421.08	449630.96
142	43+40.00	55.00 L.T.	102526.91	449632.03
143	47+12.86	55.48 L.T.	102899.99	449636.17
144	47+17.52	60.00 RT.	102902.87	449751.71
145	44+00.00	73.00 RT.	102585.49	449760.69
146	41+00.00	62.00 RT.	102297.20	449758.57
147	39+00.00	58.00 RT.	102118.46	449807.04

RIGHT OF WAY LINE TABLE		
POINT TO POINT	BEARING	DISTANCE
128 TO 139	N33°06'27"W	106.73'
139 TO 140	N17°41'35"W	106.61'
140 TO 149	N09°52'20"W	217.75'
149 TO 150	S87°42'01"W	20.00'
150 TO 151	N01°16'05"W	32.70'
151 TO 141	N89°45'49"E	20.00'
141 TO 142	N00°34'48"E	105.84'
142 TO 143	N00°38'09"E	373.10'
143 TO C $\frac{1}{4}$	N88°34'10"E	59.74'
C $\frac{1}{4}$ TO 144	N88°34'24"E	55.84'
144 TO 145	S01°37'12"E	317.51'
145 TO 146	S00°25'13"W	288.30'
146 TO 147	S15°10'17"E	185.19'
147 TO 129	S33°47'57"E	93.00'
129 TO 128	S59°50'36"W	114.59'

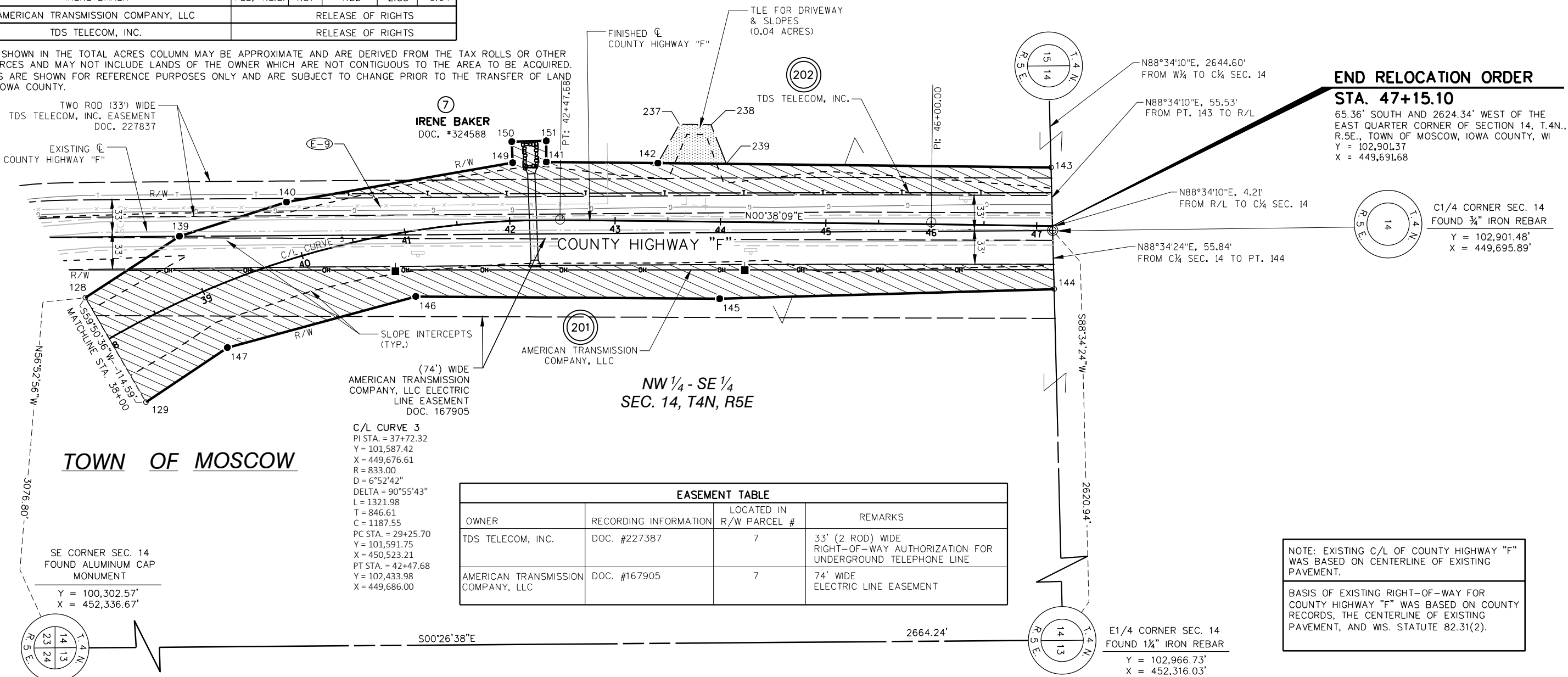
ENCROACHMENT TABLE			
ENCROACHMENT	PROPERTY OWNER	LOCATION (STATION/OFFSET)	ENCROACHMENT TYPE
E-1	PAULA SCHUETTE, BRENDA KITE, AND SARA ERICKSON	STA. 25+73 - STA. 25+87, 22.6' - 28.3', RT.	LANDSCAPE TIMBERS
E-2	GREGORY BROOKINS AND JUDY BROOKINS	STA. 26+08 - STA. 26+09, 29.9' - 34.2', LT.	RETAINING WALL
E-3	PAULA SCHUETTE, BRENDA KITE, AND SARA ERICKSON	STA. 26+37 - STA. 28+48, 21.7' - 22.0', RT.	FENCE
E-4	IRENE BAKER	STA. 28+48 - STA. 28+77, 22.0' - 33.0', RT.	FENCE
E-5	PAULA SCHUETTE, BRENDA KITE, AND SARA ERICKSON	STA. 28+39 - STA. 28+41, 22.6' - 33.0', RT.	FENCE
E-6	GEOFF BRIGGS AND JAMIE BRIGGS	STA. 32+97 - STA. 33+22, 122.4' - 138.8', LT.	RETAINING WALL
E-7	GEOFF BRIGGS AND JAMIE BRIGGS	STA. 33+69 - STA. 33+72, 150.3' - 155.1', LT.	RETAINING WALL
E-8	IRENE BAKER	STA. 35+00 - STA. 36+50, 206.0' - 210.6', LT.	FENCE
E-9	IRENE BAKER	STA. 36+71 - STA. 42+93, 15.8' - 202.6', LT.	FENCE
E-10	LARRY DEARTH AND KARYN DEARTH	STA. 400'B+33 18.2' - 25.9', LT.	RETAINING WALL

SCHEDULE OF LANDS & INTERESTS REQUIRED

PARCEL NUMBER	OWNER (S)	INTEREST REQUIRED	R/W ACRES REQUIRED			TLE ACRES REQ.
			NEW	EXISTING	TOTAL	
7	IRENE BAKER	FEE, T.L.E.	1.31	1.22	2.53	0.04
201	AMERICAN TRANSMISSION COMPANY, LLC	RELEASE OF RIGHTS				
202	TDS TELECOM, INC.	RELEASE OF RIGHTS				

NOTE: AREAS SHOWN IN THE TOTAL ACRES COLUMN MAY BE APPROXIMATE AND ARE DERIVED FROM THE TAX ROLLS OR OTHER AVAILABLE SOURCES AND MAY NOT INCLUDE LANDS OF THE OWNER WHICH ARE NOT CONTIGUOUS TO THE AREA TO BE ACQUIRED. OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO IOWA COUNTY.

**NE 1/4 - SW 1/4
SEC. 14, T4N, R5E**



**END RELOCATION ORDER
STA. 47+15.10**
65.36' SOUTH AND 2624.34' WEST OF THE EAST QUARTER CORNER OF SECTION 14, T.4N., R.5E., TOWN OF MOSCOW, IOWA COUNTY, WI
Y = 102,901.37
X = 449,691.68

C1/4 CORNER SEC. 14
FOUND 3/4" IRON REBAR
Y = 102,901.48'
X = 449,695.89'

E1/4 CORNER SEC. 14
FOUND 1 1/2" IRON REBAR
Y = 102,966.73'
X = 452,316.03'

NOTE: EXISTING C/L OF COUNTY HIGHWAY "F" WAS BASED ON CENTERLINE OF EXISTING PAVEMENT.

BASIS OF EXISTING RIGHT-OF-WAY FOR COUNTY HIGHWAY "F" WAS BASED ON COUNTY RECORDS, THE CENTERLINE OF EXISTING PAVEMENT, AND WIS. STATUTE 82.31(2).

EASEMENT TABLE			
OWNER	RECORDING INFORMATION	LOCATED IN R/W PARCEL #	REMARKS
TDS TELECOM, INC.	DOC. #227387	7	33' (2 ROD) WIDE RIGHT-OF-WAY AUTHORIZATION FOR UNDERGROUND TELEPHONE LINE
AMERICAN TRANSMISSION COMPANY, LLC	DOC. #167905	7	74' WIDE ELECTRIC LINE EASEMENT

C/L CURVE 3
PI STA. = 37+72.32
Y = 101,587.42
X = 449,676.61
R = 833.00
D = 6°52'42"
DELTA = 90°55'43"
L = 1321.98
T = 846.61
C = 1187.55
PC STA. = 29+25.70
Y = 101,591.75
X = 450,523.21
PT STA. = 42+47.68
Y = 102,433.98
X = 449,686.00

TOWN OF MOSCOW

SE CORNER SEC. 14
FOUND ALUMINUM CAP MONUMENT
Y = 100,302.57'
X = 452,336.67'

REVISION DATE 12-18-2020 08-13-2021	DATE: SEPTEMBER 11, 2019	SCALE: FEET 0 50 100	HWY: CTH F	STATE R/W PROJECT NUMBER: 5034-00-02	PLAT SHEET 4.04
GRID FACTOR N/A			COUNTY: IOWA	CONSTRUCTION PROJECT NUMBER:	PS&E SHEET E

R/W PROJECT NUMBER 5034-00-03	SHEET NUMBER 4.01	TOTAL SHEETS 21
FEDERAL PROJECT NUMBER		
PLAT OF RIGHT-OF-WAY REQUIRED FOR BLANCHARDVILLE - DANE COUNTY LINE (STH 78 TO STH 39)		
CTH F		IOWA COUNTY
CONSTRUCTION PROJECT NUMBER 5034-00-72		

CONVENTIONAL ABBREVIATIONS

ACCESS POINT/ DRIVEWAY CONNECTION	AP	PROPERTY LINE	PL
ACCESS RIGHTS	AR	RECORDED AS	(100')
ACRES	AC.	REFERENCE LINE	R/L
AND OTHERS	ET.AL.	RELEASE OF RIGHTS	ROR
BARN	B.	REMAINING	REM.
CENTERLINE	C/L	RIGHT-OF-WAY	R/W
CERTIFIED SURVEY MAP	CSM	SECTION	SEC.
CORNER	COR.	SHED	S.
CONVEYANCE OF RIGHTS	CR	STATION	STA.
DOCUMENT	DOC.	TEMPORARY LIMITED EASEMENT	TLE
EASEMENT	EASE.	VOLUME	V.
GARAGE	G.	CURVE DATA	
HIGHWAY EASEMENT	H.E.	LONG CHORD	LCH
HOUSE	H.	LONG CHORD BEARING	LCB
HOUSE TRAILER	H.T.	RADIUS	R
LAND CONTRACT	LC	DEGREE OF CURVE	D
MONUMENT	MON.	CENTRAL ANGLE OR DELTA	DELTA
PAGE	P.	LENGTH OF CURVE	L
PERMANENT LIMITED EASEMENT	PLE	TANGENT	TAN

CONVENTIONAL SYMBOLS

FOUND SURVEY MONUMENT (WITH POINT NUMBER)	○ 1040	PROPOSED R/W LINE	———
R/W MONUMENT	○ ● (SET)	EXISTING H.E. LINE	———
R/W STANDARD	△ ▲ (SET)	PROPERTY LINE	———
SIGN	ISIGN	LOT & TIE LINES	———
SECTION CORNER MONUMENT	⊕	SLOPE INTERCEPTS	////
SECTION CORNER SYMBOL	⊕	CORPORATE LIMITS	////
FEE (HATCH VARIES)	////	NO ACCESS (BY PREVIOUS ACQUISITION/CONTROL)	●●●●
TEMPORARY LIMITED EASEMENT	////	NO ACCESS (BY ACQUISITION)	
PERMANENT LIMITED EASEMENT	////	NO ACCESS (BY STATUTORY AUTHORITY)	●●●●●●
R/W BOUNDARY POINT	⊕	SECTION LINE	———
PARCEL NUMBER	⑧	QUARTER LINE	———
UTILITY PARCEL NUMBER	⑨	SIXTEENTH LINE	———
SIGN NUMBER (OFF PREMISE)	⑩	EXISTING CENTERLINE	———
BUILDING	⊕	PROPOSED REFERENCE LINE	———
		PARALLEL OFFSET	———
		ENCROACHMENT	———
		HIGHWAY EASEMENT	———

CONVENTIONAL UTILITY SYMBOLS

WATER	—W—	SANITARY SEWER	—SAN—
GAS	—G—	STORM SEWER	—SS—
TELEPHONE	—T—		
OVERHEAD TRANSMISSION LINES	—OH—	NON COMPENSABLE	⊕
ELECTRIC	—E—	COMPENSABLE	⊕
CABLE TELEVISION	—TV—	TELEPHONE POLE	⊕
FIBER OPTIC	—FO—	TELEPHONE PEDESTAL	⊕
		ELECTRIC TOWER	⊕

NOTES

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COUNTY COORDINATES, IOWA COUNTY, NAD 83 (2011) IN US SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 MONUMENTS (TYPICALLY 3/4 X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS OF PUBLIC RECORD."

FOR CURRENT ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN MADISON.

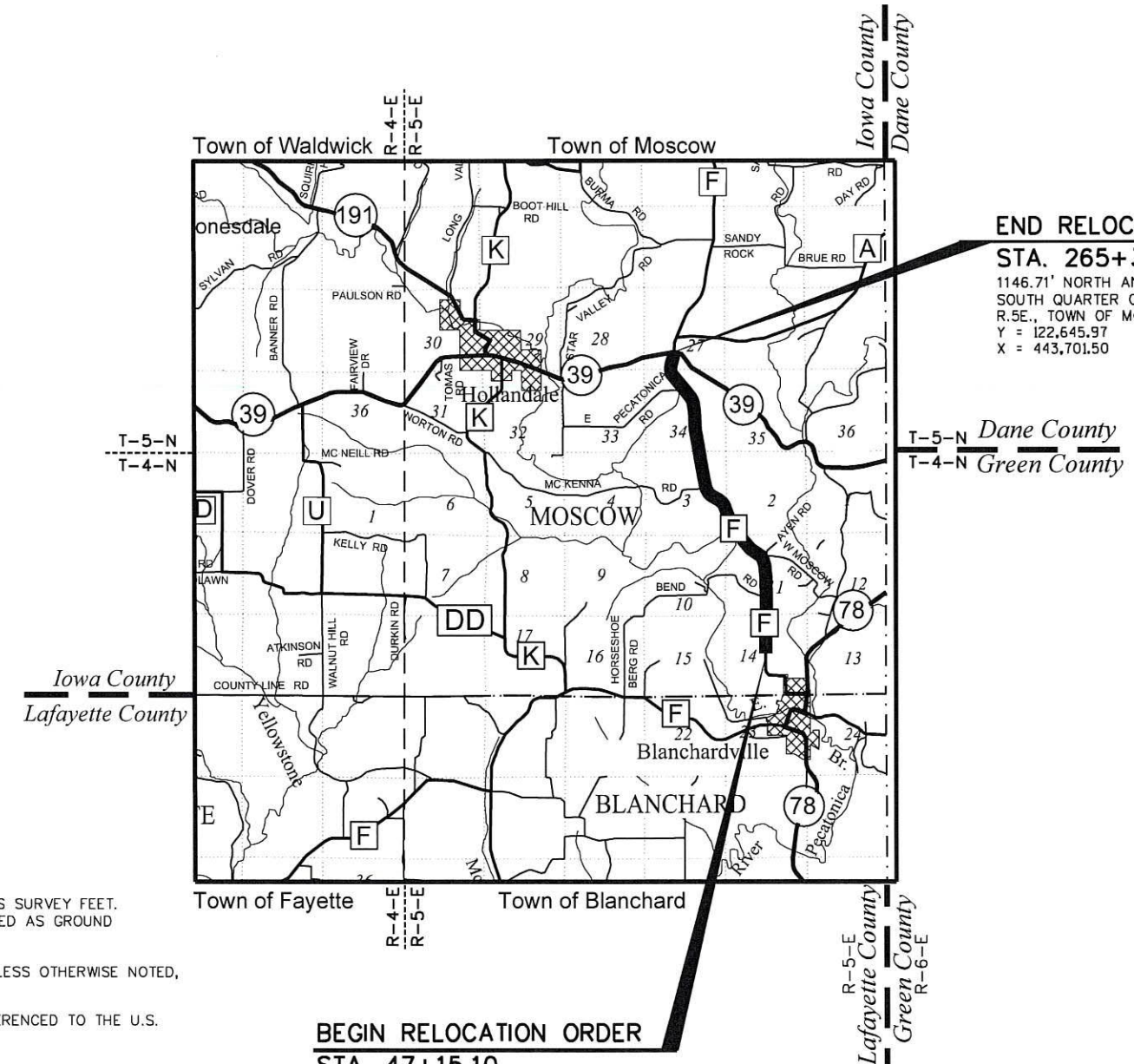
ALL RIGHT-OF-WAY LINES DEPICTED IN THE NON-ACQUISITION AREAS ARE INTENDED TO RE-ESTABLISH EXISTING RIGHT-OF-WAY LINES AS DETERMINED FROM PREVIOUS PROJECTS, OTHER RECORDED DOCUMENTS, OR FROM CENTERLINE OF EXISTING PAVEMENTS.

DIMENSIONING FOR THE NEW RIGHT-OF-WAY IS MEASURED ALONG AND PERPENDICULAR TO THE NEW REFERENCE LINES.

A TEMPORARY LIMITED EASEMENT (TLE) IS A RIGHT FOR CONSTRUCTION PURPOSES, AS DEFINED HEREIN, INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON, THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM DESIRABLE. ALL (TLEs) ON THIS PLAT EXPIRE AT THE COMPLETION OF THE CONSTRUCTION PROJECT FOR WHICH THIS INSTRUMENT IS GIVEN.

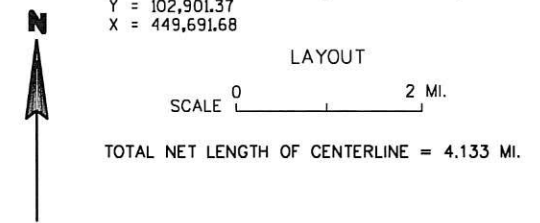
AN EASEMENT FOR HIGHWAY PURPOSES (HE), AS LONG AS SO USED. INCLUDING THE RIGHT-OF-WAY TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM DESIRABLE.

THIS PLAT IS A GRAPHIC REPRESENTATION AND IS FOR REFERENCE PURPOSE ONLY. DEEDS MUST BE CHECKED TO DETERMINE PROPERTY BOUNDARIES AND ACCESS RIGHTS.



END RELOCATION ORDER
STA. 265+36.12
 1146.71' NORTH AND 694.69' WEST OF THE SOUTH QUARTER CORNER OF SECTION 27, T.5N., R.5E., TOWN OF MOSCOW, IOWA COUNTY, WI
 Y = 122,645.97
 X = 443,701.50

BEGIN RELOCATION ORDER
STA. 47+15.10
 65.36' SOUTH AND 2624.34' WEST OF THE EAST QUARTER CORNER OF SECTION 14, T.4N., R.5E., TOWN OF MOSCOW, IOWA COUNTY, WI
 Y = 102,901.37
 X = 449,691.68



JEWELL
 associates engineers, inc.
 Engineers - Architects - Surveyors

560 SUNRISE DRIVE
 SPRING GREEN, WI 53588
 PHONE : 608.588.7484
 FAX : 608.588.9322

I HEREBY CERTIFY THAT THIS PLAT WAS MADE FOR IOWA COUNTY, WISCONSIN AND IS CORRECT TO THE BEST OF MY KNOWLEDGE AND BELIEF.

WISCONSIN
 WESLEY L. KRAEMER
 S-3026
 SPRING GREEN, WIS.
 5/14/2021
 LAND SURVEYOR

APPROVED FOR IOWA COUNTY
 DATE: 5/14/2021
 (NAME/TITLE)
Heatherly Commissioner

REVISION DATE
 8-19-21 (N.C.)

SCHEDULE OF LANDS & INTERESTS REQUIRED

PARCEL NUMBER	OWNER (S)	INTEREST REQUIRED	HE ACRES REQUIRED NEW	TLE ACRES OR S.F. REQ.
1	IRENE BAKER	HE, TLE	0.14	0.33
2	JASON M. GRUENEFELDER AND KRIS M. HOFFMASTER, AS JOINT TENANTS WITH RIGHTS OF SURVIVORSHIP	HE, TLE	0.07	0.01
3	JASON GRUENEFELDER AND KRIS GRUENEFELDER, AS SURVIVORSHIP MARITAL PROPERTY	HE, TLE	0.03	0.009
201	TDS TELECOM	RELEASE OF RIGHTS, TEMP. CONST. EASE.		
202	AMERICAN TRANSMISSION COMPANY	RELEASE OF RIGHTS, TEMP. CONST. EASE.		

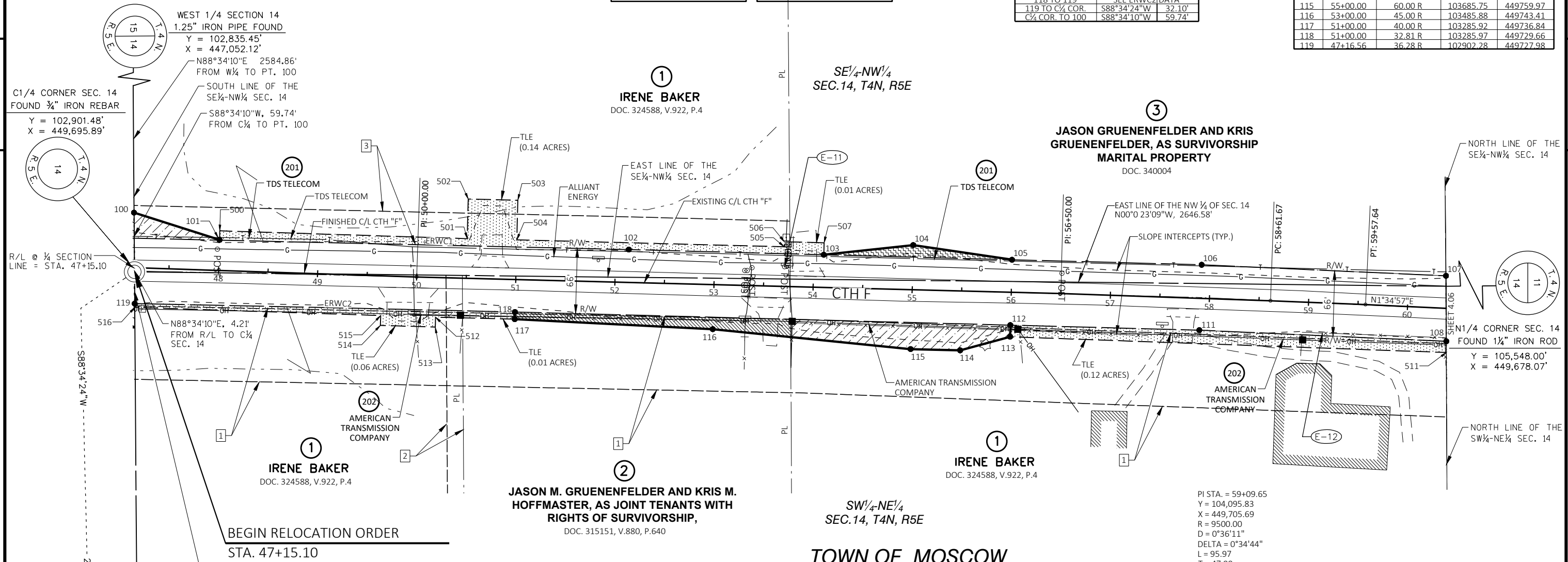
NOTE: OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO IOWA COUNTY.

ENCROACHMENT TABLE			
ENCROACHMENT	PROPERTY OWNER	LOCATION (STATION/OFFSET)	ENCROACHMENT TYPE
E-11	IRENE BAKER	STA. 53+71 23' LT.	POST
E-12	IRENE BAKER	STA. 58+95 - STA. 60+41, 28' RT. - 33' RT.	FENCE

ERWC1 CURVE DATA PT. 101 TO PT. 102	ERWC2 CURVE DATA PT. 118 TO PT. 119
ARC LENGTH = 413.00' RADIUS = 67147.61' DELTA = 000°21'09" CHORD BEARING = N00°20'05"E CHORD LENGTH = 413.00'	ARC LENGTH = 383.70' RADIUS = 67081.61' DELTA = 000°19'40" CHORD BEARING = S00°15'02"W CHORD LENGTH = 383.70'

RIGHT OF WAY LINE TABLE		
POINT TO POINT	BEARING	DISTANCE
100 TO 101	N16°40'40"E	90.56'
101 TO 102	SEE ERWC1 DATA	
102 TO 103	N00°30'40"E	196.76'
103 TO 104	N07°11'04"W	90.80'
104 TO 105	N07°26'34"E	100.75'
105 TO 106	N00°30'40"E	191.37'
106 TO 107	N01°33'58"E	246.72'
107 TO 1/4 SEC. LINE	N88°27'39"E	10.51'
1/4 SEC. LINE TO 108	N88°44'30"E	55.57'
108 TO 111	S01°33'58"W	249.42'
111 TO 112	S00°30'40"W	190.69'
112 TO 113	S89°33'08"E	11.73'
113 TO 114	S16°15'05"E	52.20'
114 TO 115	S00°26'52"W	50.00'
115 TO 116	S04°44'13"W	200.56'
116 TO 117	S01°52'48"W	200.06'
117 TO 118	N89°35'08"W	7.19'
118 TO 119	SEE ERWC2 DATA	
119 TO C/4 COR.	S88°34'24"W	32.10'
C/4 COR. TO 100	S88°34'10"W	59.74'

COORDINATE TABLE - NEW R/W POINTS				
PT.#	STATION	OFFSET	Y	X
100	47+12.86	55.48 L	102899.99	449636.17
101	48+00.00	30.83 L	102986.73	449662.16
102	52+13.24	33.16 L	103399.73	449664.57
103	54+10.00	32.94 L	103596.48	449666.33
104	55+00.00	45.00 L	103686.57	449654.97
105	56+00.00	32.73 L	103786.47	449668.02
106	57+91.04	33.89 L	103977.83	449669.73
107	60+37.42	32.76 L	104224.45	449676.47
108	60+40.75	33.24 R	104225.96	449742.54
111	57+91.00	32.11 R	103976.64	449735.72
112	56+00.00	33.27 R	103785.96	449734.02
113	56+00.00	45.00 R	103785.87	449745.75
114	55+50.00	60.00 R	103735.75	449760.36
115	55+00.00	60.00 R	103685.75	449759.97
116	53+00.00	45.00 R	103485.88	449743.41
117	51+00.00	40.00 R	103285.92	449736.84
118	51+00.00	32.81 R	103285.97	449729.66
119	47+16.56	36.28 R	102902.28	449727.98



EASEMENT TABLE				
EASEMENT NUMBER	OWNER	RECORDING INFORMATION	LOCATED IN R/W PARCEL #	REMARKS
1	AMERICAN TRANSMISSION COMPANY	DOC.167905, V.400, P.727	2	74' WIDE EASEMENT
2	TDS TELECOM	DOC. 227002, V.536, P.823	1	1 ROD EASEMENT
3	TDS TELECOM	DOC. 227837, V.539, P.021	1	2 ROD EASEMENT

NOTE: EXISTING C/L OF CTH F BASED ON CENTERLINE OF EXISTING PAVEMENT.
EXISTING RIGHT-OF-WAY FOR CTH F BASED ON THE CENTERLINE OF EXISTING PAVEMENT AND WS. STATUTE 82.31(2).

PI STA. = 59+09.65
Y = 104,095.83
X = 449,705.69
R = 9500.00
D = 0°36'11"
DELTA = 0°34'44"
L = 95.97
T = 47.99
C = 95.97
PC STA. = 58+61.67
Y = 104,047.85
X = 449,704.85
PT STA. = 59+57.64
Y = 104,143.80
X = 449,707.02

COORDINATE TABLE - TLE POINTS				
PT.#	STATION	OFFSET	Y	X
500	48+00.00	40.00 L	102986.87	449652.99
501	50+50.00	40.00 L	103236.55	449656.45
502	50+50.00	80.00 L	103236.86	449616.46
503	51+00.00	80.00 L	103286.86	449616.85
504	51+00.00	40.00 L	103286.55	449656.85
505	53+70.00	40.00 L	103556.54	449658.96
506	53+70.00	45.00 L	103556.58	449653.96
507	54+10.00	45.00 L	103596.58	449654.27
511	60+41.31	44.50 R	104226.21	449753.81
512	50+20.00	40.00 R	103205.92	449736.22
513	50+20.00	50.00 R	103205.84	449746.22
514	49+65.00	50.00 R	103150.47	449745.52
515	49+65.00	40.00 R	103150.63	449735.52
516	47+16.71	40.00 R	102902.37	449731.70

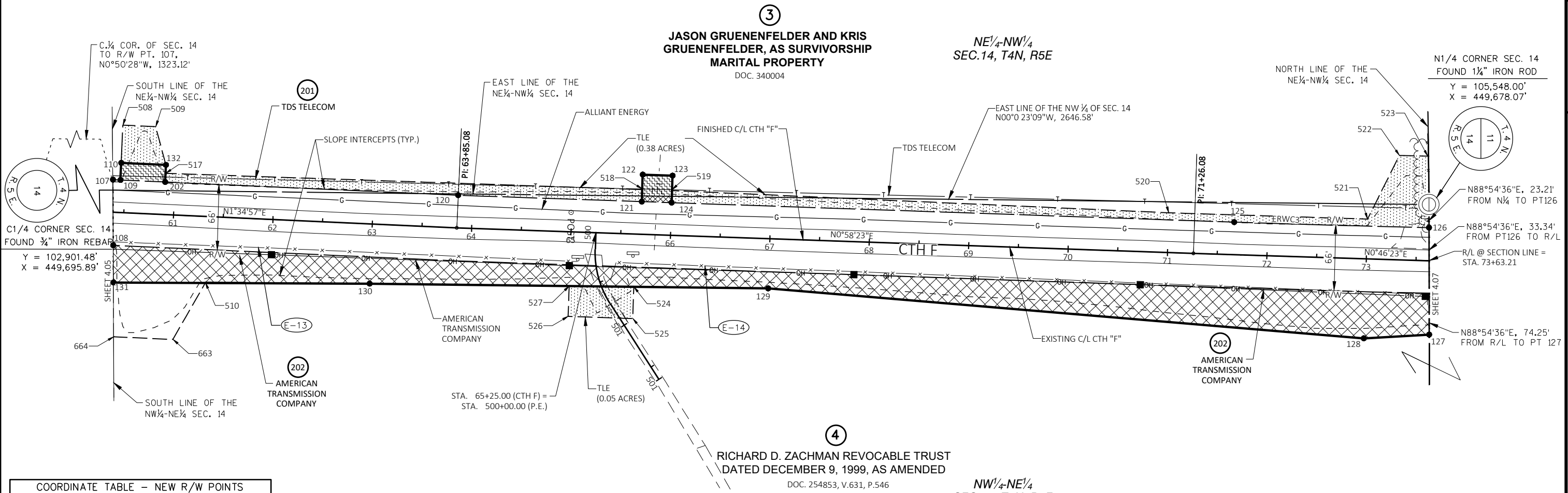
REVISION DATE 8-19-21	DATE: MAY 14, 2021	SCALE: FEET 0 50 100	HWY: CTH F	R/W PROJECT NUMBER: 5034-00-03	PLAT SHEET 4.05
GRID FACTOR N/A			COUNTY: IOWA	CONSTRUCTION PROJECT NUMBER: 5034-00-72	PS&E SHEET E

SCHEDULE OF LANDS & INTERESTS REQUIRED

PARCEL NUMBER	OWNER (S)	INTEREST REQUIRED	HE ACRES	TLE ACRES
			REQUIRED NEW	OR S.F. REQ.
3	JASON GRUENEFELDER AND KRIS GRUENEFELDER, AS SURVIVORSHIP MARITAL PROPERTY	HE, TLE	0.03	0.20
4	RICHARD D. ZACHMAN REVOCABLE TRUST, DATED DECEMBER 9, 1999, AS AMENDED	HE, TLE	0.89	0.33
201	TDS TELECOM	RELEASE OF RIGHTS, TEMP. CONST. EASE.		
202	AMERICAN TRANSMISSION COMPANY	RELEASE OF RIGHTS		

NOTE: OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO IOWA COUNTY.

ENCROACHMENT TABLE			
ENCROACHMENT	PROPERTY OWNER	LOCATION (STATION/OFFSET)	ENCROACHMENT TYPE
E-13	RICHARD ZACHMAN REVOCABLE TRUST	STA. 60+41 - STA. 65+07, 28' RT. - 33' RT.	FENCE
E-14	RICHARD ZACHMAN REVOCABLE TRUST	STA. 65+45 - STA. 72+98, 28' RT. - 33' RT.	FENCE



COORDINATE TABLE - NEW R/W POINTS				
PT.#	STATION	OFFSET	Y	X
107	60+37.42	32.76 L	104224.45	449676.47
108	60+40.75	33.24 R	104225.96	449742.54
109	60+45.00	32.76 L	104232.03	449676.68
110	60+45.00	50.00 L	104232.51	449659.45
120	63+84.90	32.86 L	104571.81	449685.97
121	65+70.00	32.86 L	104756.53	449689.11
122	65+70.00	60.00 L	104756.99	449661.97
123	66+00.00	60.00 L	104786.98	449662.48
124	66+00.00	32.86 L	104786.52	449689.61
125	71+66.17	32.72 L	105352.50	449699.23
126	73+62.12	33.32 L	105548.44	449701.27
127	73+65.62	74.21 R	105550.49	449808.84
128	73+00.00	80.00 R	105484.79	449813.74
129	67+00.00	50.00 R	104885.10	449774.16
130	63+00.00	60.00 R	104484.37	449776.45
131	60+42.60	70.50 R	104226.78	449779.84
132	60+90.00	50.00 L	104277.49	449660.69
202	60+90.00	32.78 L	104277.01	449677.91

RIGHT OF WAY LINE TABLE		
POINT TO POINT	BEARING	DISTANCE
107 TO 109	N01°33'58"E	7.58'
109 TO 110	N88°25'03"W	17.24'
110 TO 132	N01°34'57"E	45.00'
132 TO 202	S88°25'03"E	17.22'
202 TO 120	N01°33'59"E	294.90'
120 TO 121	N00°58'23"E	184.75'
121 TO 122	N89°01'37"W	27.14'
122 TO 123	N00°58'23"E	30.00'
123 TO 124	S89°01'37"E	27.14'
124 TO 125	N00°58'23"E	566.06'
125 TO 126	SEE ERWC3 DATA	
126 TO 127	N88°54'36"E	107.59'
127 TO 128	S04°15'56"E	65.87'
128 TO 129	S03°46'35"W	601.00'
129 TO 130	S00°19'37"E	400.74'
130 TO 131	S00°45'12"E	257.62'
131 TO 132	S88°44'30"W	37.31'
132 TO 108	S88°44'30"W	55.57'
108 TO 1/4 SEC. LINE	S88°27'39"W	10.51'

ERWC3 CURVE DATA
PT. 125 TO PT. 126
ARC LENGTH = 195.95'
RADIUS = 14967.00'
DELTA = 000°45'01"
CHORD BEARING = N00°35'52"E
CHORD LENGTH = 195.95'

COORDINATE TABLE - TLE POINTS				
PT.#	STATION	OFFSET	Y	X
508	60+45.00	88.00 L	104233.55	449621.46
509	60+78.00	88.00 L	104266.55	449622.37
510	61+35.86	66.61 R	104319.12	449778.49
517	60+90.00	41.50 L	104277.25	449669.19
518	65+70.00	45.00 L	104756.73	449676.97
519	66+00.00	45.00 L	104786.73	449677.48
520	71+00.00	40.00 L	105286.57	449690.97
521	73+00.00	40.00 L	105486.41	449693.75
522	73+30.00	105.00 L	105517.29	449629.16
523	73+59.31	105.15 L	105546.60	449629.41
524	65+65.00	53.06 R	104750.07	449774.93
525	65+65.00	85.00 R	104749.53	449806.86
526	65+00.00	85.00 R	104684.54	449805.76
527	65+00.00	54.54 R	104685.05	449775.30
663	61+05.00	125.00 R	104287.65	449836.04
664	60+45.30	125.00 R	104227.98	449834.39

NOTE: EXISTING C/L OF CTH F BASED ON CENTERLINE OF EXISTING PAVEMENT.
EXISTING RIGHT-OF-WAY FOR CTH F BASED ON THE CENTERLINE OF EXISTING PAVEMENT AND WS. STATUTE 82.31(2).

REVISION DATE 8-19-21	DATE: MAY 14, 2021	SCALE, FEET 0 50 100	HWY: CTH F	R/W PROJECT NUMBER: 5034-00-03	PLAT SHEET 4.06
GRID FACTOR N/A			COUNTY: IOWA	CONSTRUCTION PROJECT NUMBER: 5034-00-72	PS&E SHEET E

SCHEDULE OF LANDS & INTERESTS REQUIRED

PARCEL NUMBER	OWNER (S)	INTEREST REQUIRED	HE ACRES REQUIRED	TLE ACRES OR S.F. REQ.
			NEW	
4	RICHARD D. ZACHMAN REVOCABLE TRUST, DATED DECEMBER 9, 1999, AS AMENDED	HE, TLE	1.10	0.21
5	RICHARD J. RETRUM, JR. AND SUE M. RETRUM, HUSBAND AND WIFE AS SURVIVORSHIP MARITAL PROPERTY	HE, TLE	0.29	0.06
6	STEVEN S. ZIMMERMAN AND STEPHANIE J. ZIMMERMAN, HUSBAND AND WIFE AS SURVIVORSHIP MARITAL PROPERTY	HE, TLE	0.006	0.03
8	RICHARD J. RETRUM, JR. AND SUE M. RETRUM, HUSBAND AND WIFE	HE, TLE	0.08	0.02
201	TDS TELECOM	RELEASE OF RIGHTS, TEMP. CONST. EASE.		
202	AMERICAN TRANSMISSION COMPANY	RELEASE OF RIGHTS, TEMP. CONST. EASE.		

NOTE: OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO IOWA COUNTY.

ENCROACHMENT TABLE			
ENCROACHMENT	PROPERTY OWNER	LOCATION (STATION/OFFSET)	ENCROACHMENT TYPE
E-15	RICHARD RETRUM JR & SUSAN RETRUM	STA. 81+13 - STA. 84+74, 28' LT. - 33' LT.	FENCE
E-16	RICHARD ZACHMAN REVOCABLE TRUST	STA. 401'E+88 - STA. 402'E+92, 28' RT. - 33' RT.	FENCE

5
RICHARD J. RETRUM, JR. AND SUE M. RETRUM,
HUSBAND AND WIFE AS SURVIVORSHIP
MARITAL PROPERTY,
 DOC. 211918, V.500, P.315

SE 1/4-SW 1/4
SEC.11, T4N, R5E

NE 1/4-SW 1/4
SEC.11, T4N, R5E
8
RICHARD J. RETRUM, JR.
AND SUE M. RETRUM,
HUSBAND AND WIFE
 DOC. 211908, V.500, P.290

4
RICHARD D. ZACHMAN REVOCABLE TRUST
DATED DECEMBER 9, 1999, AS AMENDED
 DOC. 254853, V.631, P.546

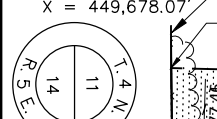
TOWN OF MOSCOW

SW 1/4-SE 1/4
SEC.11, T4N, R5E

6
STEVEN S. ZIMMERMAN AND
STEPHANIE J. ZIMMERMAN,
HUSBAND AND WIFE AS
SURVIVORSHIP MARITAL
PROPERTY,
 DOC. 191769, V.452, P.419
NW 1/4-SE 1/4
SEC.11, T4N, R5E

4
 S 1/4 CORNER SEC. 11
 FOUND 1 1/4" IRON ROD

Y = 105,548.00'
 X = 449,678.07'



SOUTH LINE OF THE
 SE 1/4-SW 1/4 SEC. 11

N88°54'36"E, 23.21'
 FROM S 1/4 TO PT. 126

TLE (0.06 ACRES)

N88°54'36"E, 33.34'
 FROM PT126 TO R/L

N88°54'36"E, 74.25'
 FROM R/L TO PT 127

SOUTH LINE OF THE
 SW 1/4-SE 1/4 SEC. 11

N00°46'23"E

N00°29'34"E, 5244.55'

N0°10'25"W

N0°39'04"W

N0°10'09"W

N12°27'45"E

N09°10'55"W

N07°52'46"E

N17°03'13"W

N79°32'56"W

N10°47'15"E

N10°47'15"E

S86°29'38"E

N01°50'18"E

N89°20'56"E

S00°58'22"E

N89°20'56"E

S03°49'20"E

S05°32'13"W

S04°15'56"E

S88°54'36"W

105.15 L

105.585.55

105610.70

105720.72

105750.72

106859.02

106865.85

106936.34

106936.46

105.15 L

105.585.55

105610.70

105720.72

105750.72

106859.02

106865.85

106936.34

106936.46

RIGHT OF WAY LINE TABLE			
POINT TO POINT	BEARING	DISTANCE	
126 TO 133	SEE ERWC4 DATA		
133 TO 134	N00°10'09"W	69.94'	
134 TO 135	S89°49'51"W	26.80'	
135 TO 136	N00°10'25"W	30.00'	
136 TO 137	N89°49'51"E	26.80'	
137 TO 138	N00°10'09"W	135.00'	
138 TO 139	S89°49'51"W	11.81'	
139 TO 140	N05°53'04"W	401.99'	
140 TO 141	N12°27'45"E	204.62'	
141 TO 142	N09°10'55"W	101.12'	
142 TO 143	N07°52'46"E	101.12'	
143 TO 144	N17°03'13"W	89.56'	
144 TO 145	SEE ERWC5 DATA		
145 TO 146	N79°32'56"W	22.23'	
146 TO 147	N10°47'15"E	66.00'	
147 TO 148	N10°47'15"E	26.63'	
148 TO 149	S86°29'38"E	82.57'	
149 TO 150	N01°50'18"E	65.06'	
150 TO 151	N89°20'56"E	88.69'	
151 TO 152	S00°58'22"E	250.00'	
152 TO 153	N89°20'56"E	17.08'	
153 TO 154	S03°49'20"E	501.42'	
154 TO 155	S05°32'13"W	301.50'	
155 TO 127	S04°15'56"E	336.46'	
127 TO 126	S88°54'36"W	107.59'	

ERWC4 CURVE DATA PT. 126 TO PT. 133	
ARC LENGTH = 102.39'	
RADIUS = 14967.00'	
DELTA = 00°23'31"	
CHORD BEARING = N00°01'37"E	
CHORD LENGTH = 102.39'	

ERWC5 CURVE DATA PT. 144 TO PT. 145	
ARC LENGTH = 74.34'	
RADIUS = 633.00'	
DELTA = 006°43'42"	
CHORD BEARING = N82°54'47"W	
CHORD LENGTH = 74.29'	

COORDINATE TABLE - TLE POINTS				
PT.#	STATION	OFFSET	Y	X
523	73+59.31	105.15 L	105546.60	449629.41
528	74+00.00	105.00 L	105585.55	449629.73
529	74+25.00	55.00 L	105610.70	449679.65
530	75+35.00	50.00 L	105720.72	449684.32
531	75+65.00	50.00 L	105750.72	449684.23
532	401'E+30.00	33.05 L	106859.02	449525.90
533	401'E+30.00	40.00 L	106865.85	449527.21
534	87+50.00	44.57 R	106936.34	449771.21
535	86+50.00	55.00 R	106836.46	449782.78

NOTE: EXISTING C/L OF CTH F BASED ON CENTERLINE OF EXISTING PAVEMENT.
 EXISTING RIGHT-OF-WAY FOR CTH F BASED ON THE CENTERLINE OF EXISTING PAVEMENT AND WS. STATUTE 82.31(2).

EASEMENT TABLE				
EASEMENT NUMBER	OWNER	RECORDING INFORMATION	LOCATED IN R/W PARCEL #	REMARKS
4	TDS TELECOM	DOC. 234173, V.553, P.799	5	2 ROD EASEMENT

COORDINATE TABLE - NEW R/W POINTS				
PT.#	STATION	OFFSET	Y	X
126	73+62.12	33.32 L	105548.44	449701.27
127	73+65.62	74.21 R	105550.49	449808.84
133	74+65.06	33.21 L	105650.82	449701.32
134	75+35.00	33.20 L	105720.77	449701.11
135	75+35.00	60.00 L	105720.69	449674.32
136	75+65.00	60.00 L	105750.69	449674.23
137	75+65.00	33.20 L	105750.77	449701.02
138	77+00.00	33.19 L	105885.77	449700.63
139	77+00.00	45.00 L	105885.73	449688.82
140	81+00.00	85.00 L	106285.60	449647.60
141	83+00.00	40.00 L	106485.41	449691.76
142	84+00.00	55.00 L	106585.23	449675.63
143	85+00.00	40.00 L	106685.39	449689.49
144	85+85.91	65.29 L	106771.01	449663.22
145	402'E+06.24	32.78 R	106780.18	449589.50
146	401'E+85.00	32.63 R	106784.21	449567.64
147	401'E+85.00	33.37 L	106849.05	449579.99
148	401'E+85.00	60.00 L	106875.20	449584.98
149	86+85.00	60.00 L	106870.15	449667.39
150	87+50.00	57.17 L	106935.18	449669.47
151	87+50.00	31.52 R	106936.19	449758.16
152	85+00.00	32.92 R	106686.22	449762.41
153	85+00.00	50.00 R	106686.42	449779.48
154	80+00.00	80.00 R	106186.11	449812.91
155	77+00.00	50.00 R	105886.02	449783.82

REVISION DATE 8-19-21	DATE: MAY 14, 2021	SCALE, FEET 0 50 100	HWY: CTH F	R/W PROJECT NUMBER: 5034-00-03	PLAT SHEET 4.07
GRID FACTOR N/A			COUNTY: IOWA	CONSTRUCTION PROJECT NUMBER: 5034-00-72	PS&E SHEET E

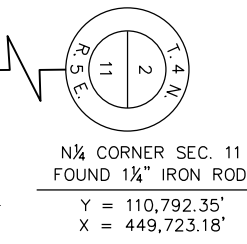
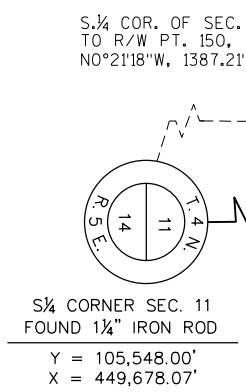
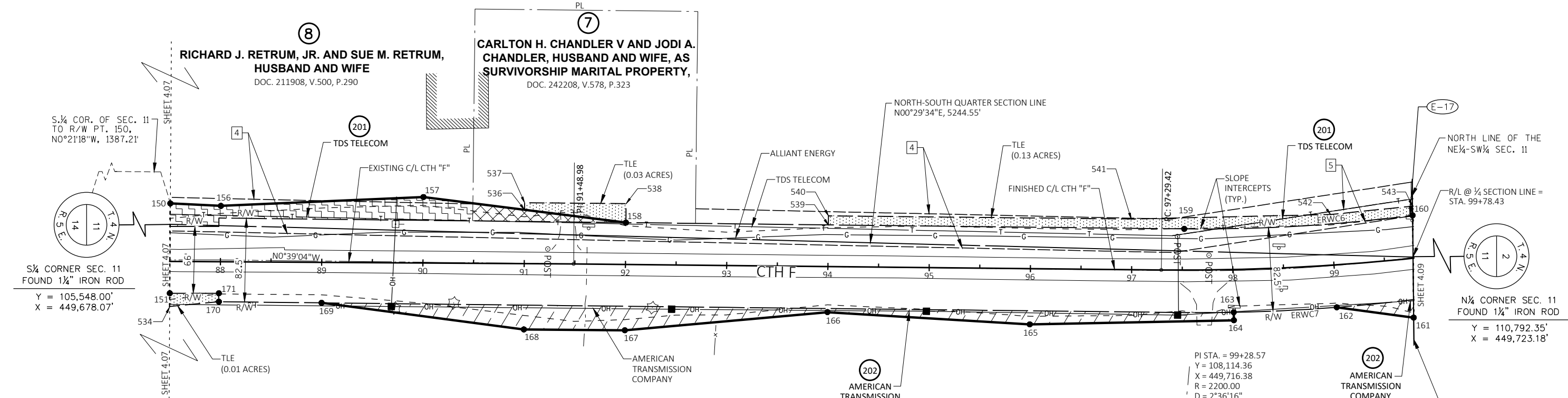
SCHEDULE OF LANDS & INTERESTS REQUIRED

PARCEL NUMBER	OWNER (S)	INTEREST REQUIRED	HE ACRES REQUIRED NEW	TLE ACRES OR S.F. REQ.
6	STEVEN S. ZIMMERMAN AND STEPHANIE J. ZIMMERMAN, HUSBAND AND WIFE AS SURVIVORSHIP MARITAL PROPERTY	HE, TLE	0.28	0.01
7	CARLTON H. CHANDLER V AND JODI A. CHANDLER, HUSBAND AND WIFE, AS SURVIVORSHIP MARITAL PROPERTY	HE, TLE	0.03	0.03
8	RICHARD J. RETRUM, JR. AND SUE M. RETRUM, HUSBAND AND WIFE	HE, TLE	0.13	0.13
201	TDS TELECOM	RELEASE OF RIGHTS, TEMP. CONST. EASE.		
202	AMERICAN TRANSMISSION COMPANY	RELEASE OF RIGHTS, TEMP. CONST. EASE.		

ENCROACHMENT TABLE			
ENCROACHMENT	PROPERTY OWNER	LOCATION (STATION/OFFSET)	ENCROACHMENT TYPE
E-17	DANNY & PEGGY SENF	STA. 99+82 - STA. 99+96, 28' LT. - 42' LT.	GATES AND POSTS

NOTE: OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO IOWA COUNTY.

NE 1/4-SW 1/4 SEC. 11, T4N, R5E



RIGHT OF WAY LINE TABLE		
POINT TO POINT	BEARING	DISTANCE
150 TO 156	N01°50'18"E	50.05'
156 TO 157	N03°30'49"W	200.25'
157 TO 158	N06°10'25"E	201.59'
158 TO 159	N00°22'22"W	551.59'
159 TO 160	SEE ERWC6 DATA	
160 TO 1/2 LINE	N88°21'28"E	41.14'
1/2 LINE TO 161	N88°21'49"E	59.76'
161 TO 162	S06°36'31"W	76.65'
162 TO 163	SEE ERWC7 DATA	
163 TO 164	N87°44'27"E	7.82'
164 TO 165	S02°10'14"E	201.69'
165 TO 166	S02°26'30"W	200.25'
166 TO 167	S06°07'53"E	201.00'
167 TO 168	S00°32'01"E	99.74'
168 TO 169	S06°28'27"W	201.56'
169 TO 170	S00°22'22"E	101.42'
170 TO 171	S89°01'38"W	8.26'
171 TO 151	S00°58'22"E	48.54'
151 TO 150	S89°20'56"W	88.69'

ERWC6 CURVE DATA	
PT. 159 TO PT. 160	
ARC LENGTH =	226.13'
RADIUS =	1608.75'
DELTA =	008°03'13"
CHORD BEARING =	N04°23'59"W
CHORD LENGTH =	225.95'

ERWC7 CURVE DATA	
PT. 162 TO PT. 163	
ARC LENGTH =	101.92'
RADIUS =	1691.25'
DELTA =	003°27'10"
CHORD BEARING =	S03°47'14"E
CHORD LENGTH =	101.90'

COORDINATE TABLE - NEW R/W POINTS				
PT.#	STATION	OFFSET	Y	X
150	87+50.00	57.17 L	106935.18	449669.47
151	87+50.00	31.52 R	106936.19	449758.16
156	88+00.00	55.00 L	106985.20	449671.08
157	90+00.00	65.00 L	107185.08	449658.81
158	92+00.00	41.25 L	107385.50	449680.49
159	97+52.01	40.67 L	107937.08	449676.90
160	99+82.39	42.21 L	108162.36	449659.57
161	99+73.19	58.27 R	108165.25	449760.43
162	99+00.00	41.77 R	108089.11	449751.61
163	98+00.00	42.18 R	107987.43	449758.34
164	98+00.00	50.00 R	107987.74	449766.16
165	96+00.00	55.00 R	107786.20	449773.80
166	94+00.00	45.00 R	107586.13	449765.26
167	92+00.00	65.00 R	107386.28	449786.73
168	91+00.00	65.00 R	107286.55	449787.66
169	89+00.00	40.00 R	107086.28	449764.94
170	87+98.58	39.51 R	106984.86	449765.60
171	87+98.54	31.25 R	106984.72	449757.34

COORDINATE TABLE - TLE POINTS				
PT.#	STATION	OFFSET	Y	X
534	87+50.00	44.57 R	106936.34	449771.21
536	91+00.00	53.03 L	107285.21	449669.64
537	91+00.00	60.00 L	107285.13	449662.67
538	92+00.00	60.00 L	107385.36	449661.74
539	94+00.00	41.08 L	107585.50	449679.19
540	94+00.00	50.00 L	107585.43	449670.27
541	97+00.00	50.00 L	107885.42	449668.06
542	99+00.00	50.00 L	108081.33	449660.17
543	99+83.26	51.23 L	108162.10	449650.51

NOTE: EXISTING C/L OF CTH F BASED ON CENTERLINE OF EXISTING PAVEMENT.
EXISTING RIGHT-OF-WAY FOR CTH F BASED ON THE CENTERLINE OF EXISTING PAVEMENT AND WS. STATUTE 82.31(2).

NW 1/4-SE 1/4 SEC. 11, T4N, R5E
TOWN OF MOSCOW

STEVEN S. ZIMMERMAN AND STEPHANIE J. ZIMMERMAN, HUSBAND AND WIFE AS SURVIVORSHIP MARITAL PROPERTY, DOC. 191769, V.452, P.419

EASEMENT TABLE				
EASEMENT NUMBER	OWNER	RECORDING INFORMATION	LOCATED IN R/W PARCEL #	REMARKS
4	TDS TELECOM	DOC. 234173, V.553, P.799	8	2 ROD EASEMENT
5	TDS TELECOM	DOC. 227002, V.536, P.823	8	1 ROD EASEMENT

REVISION DATE 8-19-21	DATE: MAY 14, 2021	SCALE, FEET 0 50 100	HWY: CTH F	R/W PROJECT NUMBER: 5034-00-03	PLAT SHEET 4.08
	GRID FACTOR N/A		COUNTY: IOWA	CONSTRUCTION PROJECT NUMBER: 5034-00-72	PS&E SHEET E

SCHEDULE OF LANDS & INTERESTS REQUIRED

PARCEL NUMBER	OWNER (S)	INTEREST REQUIRED	HE ACRES REQUIRED	TLE ACRES OR S.F. REQ.
			NEW	
6	STEVEN S. ZIMMERMAN AND STEPHANIE J. ZIMMERMAN, HUSBAND AND WIFE AS SURVIVORSHIP MARITAL PROPERTY	HE, TLE	0.19	0.13
9	DANNY R. SENF AND PEGGY L. SENF, HUSBAND AND WIFE AS SURVIVORSHIP MARITAL PROPERTY	HE, TLE	0.04	0.06
201	TDS TELECOM	RELEASE OF RIGHTS		
202	AMERICAN TRANSMISSION COMPANY	RELEASE OF RIGHTS, TEMP. CONST. EASE.		

NOTE: OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO IOWA COUNTY.

RIGHT OF WAY LINE TABLE		
POINT TO POINT	BEARING	DISTANCE
160 TO 172	SEE ERWC8 DATA	
172 TO 173	SEE ERWC9 DATA	
173 TO 174	S77°01'04"W	3.63'
174 TO 175	N15°54'28"W	200.20'
175 TO 176	N76°57'17"E	11.80'
176 TO 394	N14°17'30"W	81.83'
394 TO 177	SEE ERWC10 DATA	
177 TO 178	N72°20'39"E	99.72'
178 TO 179	S24°36'55"E	55.56'
179 TO 180	S07°35'32"E	251.18'
180 TO 181	SEE ERWC11 DATA	
181 TO 182	S14°16'01"E	304.24'
182 TO 161	S06°36'31"W	28.37'
161 TO 1/2 LINE	S88°21'49"W	59.76'
1/2 LINE TO 160	S88°21'28"W	41.14'

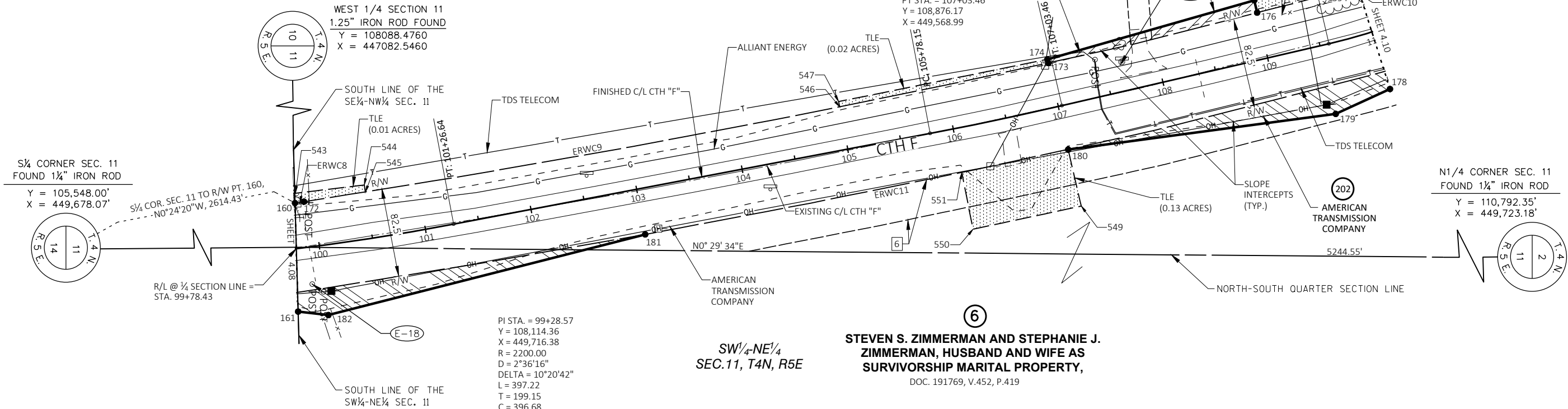
ERWC8 CURVE DATA PT. 160 TO PT. 172
ARC LENGTH = 8.46'
RADIUS = 1608.75'
DELTA = 000°18'05"
CHORD BEARING = N08°34'38"W
CHORD LENGTH = 8.46'

ERWC9 CURVE DATA PT. 172 TO PT. 173
ARC LENGTH = 704.77'
RADIUS = 11167.50'
DELTA = 003°36'57"
CHORD BEARING = N10°32'09"W
CHORD LENGTH = 704.66'

ERWC10 CURVE DATA PT. 394 TO PT. 177
ARC LENGTH = 14.62'
RADIUS = 438.75'
DELTA = 001°54'33"
CHORD BEARING = N15°14'47"W
CHORD LENGTH = 14.62'

ERWC11 CURVE DATA PT. 180 TO PT. 181
ARC LENGTH = 401.60'
RADIUS = 11250.00'
DELTA = 002°02'43"
CHORD BEARING = S11°19'33"E
CHORD LENGTH = 401.58'

ENCROACHMENT TABLE			
ENCROACHMENT	PROPERTY OWNER	LOCATION (STATION/OFFSET)	ENCROACHMENT TYPE
E-19	DANNY & PEGGY SENF	STA. 107+64 - STA. 107+70, 24.5' LT. - 30' LT.	LANDSCAPING
E-20	DANNY & PEGGY SENF	STA. 109+22 - STA. 111+07, 36.5' LT. - 40' LT.	FENCE
E-18	STEVEN & STEPHANIE ZIMMERMAN	STA. 99+89, 34.5' RT.	POST



DANNY R. SENF AND PEGGY L. SENF, HUSBAND AND WIFE AS SURVIVORSHIP MARITAL PROPERTY,
DOC. 184940, V.436, P.516

PI STA. = 111+82.06
Y = 109,342.42
X = 449,460.96
R = 515.00
D = 11°07'31"
DELTA = 46°55'10"
L = 421.73
T = 223.50
C = 410.05
PC STA. = 109+58.56
Y = 109,124.68
X = 449,511.41
PT STA. = 113+80.29
Y = 109,454.28
X = 449,267.48

PI STA. = 106+40.81
Y = 108,815.12
X = 449,583.14
R = 3150.00
D = 1°49'08"
DELTA = 2°16'45"
L = 125.31
T = 62.66
C = 125.30
PC STA. = 105+78.15
Y = 108,753.56
X = 449,594.84
PT STA. = 107+03.46
Y = 108,876.17
X = 449,568.99

PI STA. = 99+28.57
Y = 108,114.36
X = 449,716.38
R = 2200.00
D = 2°36'16"
DELTA = 10°20'42"
L = 397.22
T = 199.15
C = 396.68
PC STA. = 97+29.42
Y = 107,915.21
X = 449,717.85
PT STA. = 101+26.64
Y = 108,310.00
X = 449,679.18

COORDINATE TABLE - NEW R/W POINTS				
PT.#	STATION	OFFSET	Y	X
160	99+82.39	42.21 L	108162.36	449659.57
161	99+73.19	58.27 R	108165.25	449760.43
172	99+91.02	42.43 L	108170.73	449658.30
173	107+00.00	41.37 L	108863.51	449529.46
174	107+00.00	45.00 L	108862.69	449525.92
175	109+00.00	55.00 L	109055.22	449471.05
176	109+00.00	43.20 L	109057.89	449482.54
394	109+84.01	44.41 L	109137.18	449462.34
177	110+00.00	44.02 L	109151.29	449458.50
178	110+00.00	55.70 R	109181.53	449553.52
179	109+50.00	65.00 R	109131.02	449576.67
180	107+00.00	41.13 R	108882.04	449609.85
181	103+00.00	42.67 R	108488.28	449688.72
182	100+00.00	65.00 R	108193.43	449763.69

COORDINATE TABLE - TLE POINTS				
PT.#	STATION	OFFSET	Y	X
543	99+83.26	51.23 L	108162.10	449650.51
544	100+50.00	50.00 L	108226.85	449642.77
545	100+50.00	43.31 L	108227.87	449649.38
546	105+00.00	40.00 L	108669.32	449570.15
547	105+00.00	45.00 L	108668.38	449565.23
548	110+00.00	53.88 L	109148.30	449449.10
549	107+00.00	95.00 R	108894.14	449662.34
550	106+00.00	95.00 R	108793.41	449683.89
551	106+00.00	41.16 R	108782.98	449631.07

NOTE: EXISTING C/L OF CTH F BASED ON CENTERLINE OF EXISTING PAVEMENT.
EXISTING RIGHT-OF-WAY FOR CTH F BASED ON THE CENTERLINE OF EXISTING PAVEMENT AND WIS. STATUTE 82.31(2).

EASEMENT TABLE				
EASEMENT NUMBER	OWNER	RECORDING INFORMATION	LOCATED IN R/W PARCEL #	REMARKS
6	TDS TELECOM	DOC. 227004, V.536, P.826	6	2 ROD EASEMENT
18	TDS TELECOM	DOC. 145770, V.350, P.590	9	20' WIDE EASEMENT

REVISION DATE 8-19-21	DATE: MAY 14, 2021	SCALE, FEET 0 50 100	HWY: CTH F	R/W PROJECT NUMBER: 5034-00-03	PLAT SHEET 4.09
GRID FACTOR N/A			COUNTY: IOWA	CONSTRUCTION PROJECT NUMBER: 5034-00-72	PS&E SHEET E

COORDINATE TABLE - NEW R/W POINTS

PT.#	STATION	OFFSET	Y	X
177	110+00.00	44.02 L	109151.29	449458.50
178	110+00.00	55.70 R	109181.53	449553.52
183	113+33.20	43.84 L	109393.10	449281.80
184	114+00.00	41.58 L	109428.15	449229.60
185	114+00.00	45.00 L	109425.19	449227.89
186	114+50.00	60.00 L	109437.23	449177.10
187	118+00.00	50.00 L	109624.02	448877.86
188	120+00.00	53.00 L	109746.54	448710.77
189	122+00.00	45.00 L	109899.08	448570.87
190	123+00.00	40.00 L	109982.71	448510.52
191	123+00.00	45.76 R	110033.78	448579.41
192	122+11.06	44.68 R	109965.64	448631.98
193	119+50.00	47.00 R	109790.66	448813.54
194	117+00.00	60.00 R	109666.25	449020.73
195	116+00.00	50.00 R	109607.54	449102.30
196	116+00.00	41.54 R	109600.21	449098.06
197	114+00.42	41.13 R	109499.97	449270.64
198	113+35.06	38.64 R	109461.58	449327.80
199	112+25.00	40.28 R	109384.92	449417.87
200	111+78.34	65.00 R	109361.50	449469.86
201	111+19.67	55.00 R	109301.75	449498.44

PI STA. = 111+82.06
 Y = 109,342.42
 X = 449,460.96
 R = 515.00
 D = 11°07'31"
 DELTA = 46°55'10"
 L = 421.73
 T = 223.50
 C = 410.05
 PC STA. = 109+58.56
 Y = 109,124.68
 X = 449,511.41
 PT STA. = 113+80.29
 Y = 109,454.28
 X = 449,267.48

NOTE: EXISTING C/L OF CTH F BASED ON CENTERLINE OF EXISTING PAVEMENT.
 EXISTING RIGHT-OF-WAY FOR CTH F BASED ON THE CENTERLINE OF EXISTING PAVEMENT AND WS. STATUTE 82.31(2).

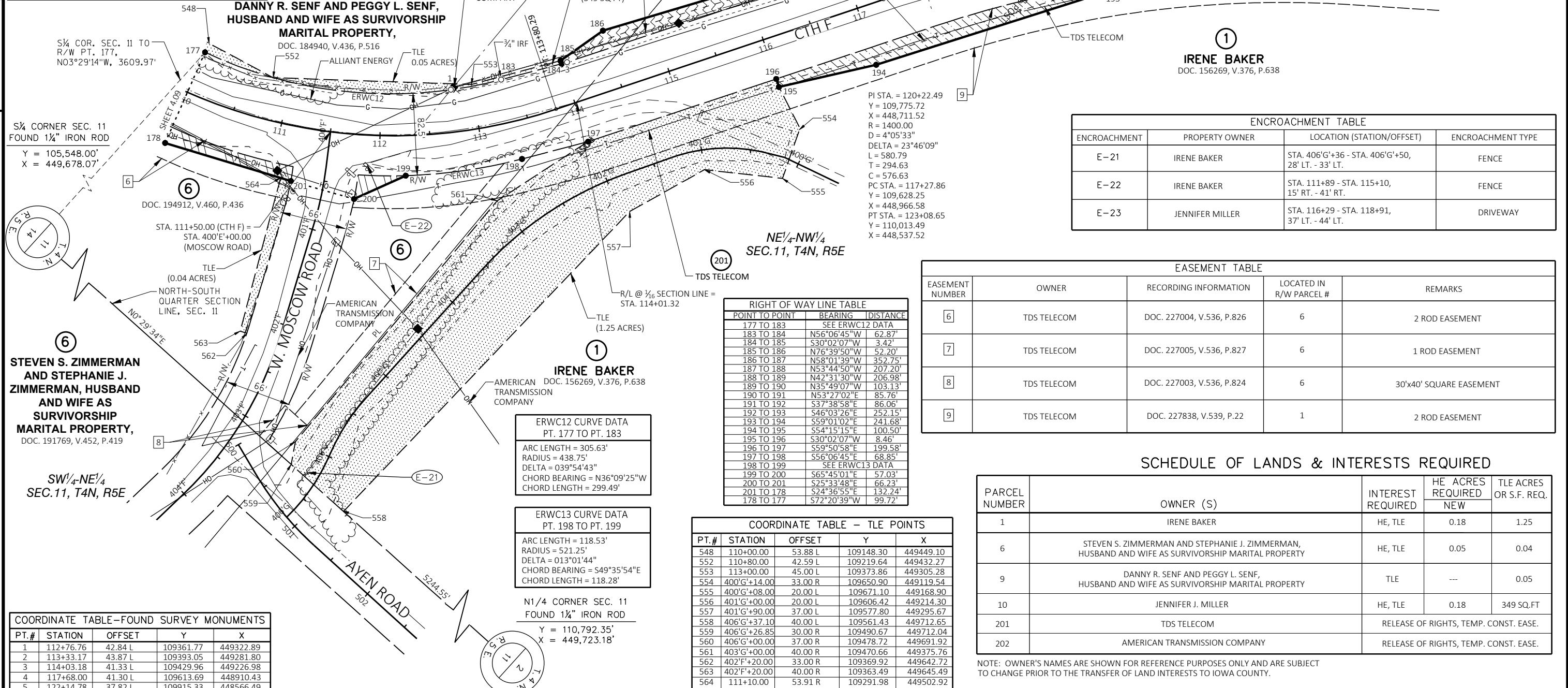
NE 1/4-NW 1/4
 SEC. 11, T4N, R5E
 JENNIFER J. MILLER,
 DOC. 309290, V.857, P.141

PLAT OF SURVEY
 BY JAMES M.
 BAKER
 DATED
 2-27-2009

PLAT OF SURVEY
 BY THOM R.
 CRENLIE
 DATED
 12-23-80, V.2,
 PG. 467

SE 1/4-NW 1/4
 SEC. 11, T4N, R5E

TOWN OF MOSCOW



ENCROACHMENT TABLE

ENCROACHMENT	PROPERTY OWNER	LOCATION (STATION/OFFSET)	ENCROACHMENT TYPE
E-21	IRENE BAKER	STA. 406'G'+36 - STA. 406'G'+50, 28' LT. - 33' LT.	FENCE
E-22	IRENE BAKER	STA. 111+89 - STA. 115+10, 15' RT. - 41' RT.	FENCE
E-23	JENNIFER MILLER	STA. 116+29 - STA. 118+91, 37' LT. - 44' LT.	DRIVEWAY

PI STA. = 120+22.49
 Y = 109,775.72
 X = 448,711.52
 R = 1400.00
 D = 4°05'33"
 DELTA = 23°46'09"
 L = 580.79
 T = 294.63
 C = 576.63
 PC STA. = 117+27.86
 Y = 109,628.25
 X = 448,966.58
 PT STA. = 123+08.65
 Y = 110,013.49
 X = 448,537.52

EASEMENT TABLE

EASEMENT NUMBER	OWNER	RECORDING INFORMATION	LOCATED IN R/W PARCEL #	REMARKS
6	TDS TELECOM	DOC. 227004, V.536, P.826	6	2 ROD EASEMENT
7	TDS TELECOM	DOC. 227005, V.536, P.827	6	1 ROD EASEMENT
8	TDS TELECOM	DOC. 227003, V.536, P.824	6	30'x40' SQUARE EASEMENT
9	TDS TELECOM	DOC. 227838, V.539, P.22	1	2 ROD EASEMENT

RIGHT OF WAY LINE TABLE

POINT TO POINT	BEARING	DISTANCE
177 TO 183	SEE ERWC12 DATA	
183 TO 184	N56°06'45"W	62.87'
184 TO 185	S30°02'07"W	3.42'
185 TO 186	N76°39'50"W	52.20'
186 TO 187	N58°01'39"W	352.75'
187 TO 188	N53°44'50"W	207.20'
188 TO 189	N42°31'30"W	206.98'
189 TO 190	N35°49'07"W	103.13'
190 TO 191	N53°27'02"E	85.76'
191 TO 192	S37°38'58"E	86.06'
192 TO 193	S46°03'26"E	252.15'
193 TO 194	S59°01'02"E	241.68'
194 TO 195	S54°15'15"E	100.50'
195 TO 196	S30°02'07"W	8.46'
196 TO 197	S59°50'58"E	199.58'
197 TO 198	S56°06'45"E	68.85'
198 TO 199	SEE ERWC13 DATA	
199 TO 200	S65°45'01"E	57.03'
200 TO 201	S25°33'48"E	66.23'
201 TO 178	S24°36'55"E	132.24'
178 TO 177	S72°20'39"W	99.72'

COORDINATE TABLE - TLE POINTS

PT.#	STATION	OFFSET	Y	X
548	110+00.00	53.88 L	109148.30	449449.10
552	110+80.00	42.59 L	109219.64	449432.27
553	113+00.00	45.00 L	109373.86	449305.28
554	400'G'+14.00	33.00 R	109650.90	449119.54
555	400'G'+08.00	20.00 L	109671.10	449168.90
556	401'G'+00.00	20.00 L	109606.42	449214.30
557	401'G'+90.00	37.00 L	109577.80	449295.67
558	406'G'+37.10	40.00 L	109561.43	449712.65
559	406'G'+26.85	30.00 R	109490.67	449712.04
560	406'G'+00.00	37.00 R	109478.72	449691.92
561	403'G'+00.00	40.00 R	109470.66	449375.76
562	402'F'+20.00	33.00 R	109369.92	449642.72
563	402'F'+20.00	40.00 R	109363.49	449645.49
564	111+10.00	53.91 R	109291.98	449502.92

**ERWC12 CURVE DATA
 PT. 177 TO PT. 183**
 ARC LENGTH = 305.63'
 RADIUS = 438.75'
 DELTA = 039°54'43"
 CHORD BEARING = N36°09'25"W
 CHORD LENGTH = 299.49'

**ERWC13 CURVE DATA
 PT. 198 TO PT. 199**
 ARC LENGTH = 118.53'
 RADIUS = 521.25'
 DELTA = 013°01'44"
 CHORD BEARING = S49°35'54"E
 CHORD LENGTH = 118.28'

**N1/4 CORNER SEC. 11
 FOUND 1 1/4" IRON ROD**
 Y = 110,792.35'
 X = 449,723.18'

COORDINATE TABLE-FOUND SURVEY MONUMENTS

PT.#	STATION	OFFSET	Y	X
1	112+76.76	42.84 L	109361.77	449322.89
2	113+33.17	43.87 L	109393.05	449281.80
3	114+03.18	41.33 L	109429.96	449226.98
4	117+68.00	41.30 L	109613.69	448910.43
5	122+14.78	37.82 L	109915.33	448566.49

SCHEDULE OF LANDS & INTERESTS REQUIRED

PARCEL NUMBER	OWNER (S)	INTEREST REQUIRED	HE ACRES REQUIRED NEW	TLE ACRES OR S.F. REQ.
1	IRENE BAKER	HE, TLE	0.18	1.25
6	STEVEN S. ZIMMERMAN AND STEPHANIE J. ZIMMERMAN, HUSBAND AND WIFE AS SURVIVORSHIP MARITAL PROPERTY	HE, TLE	0.05	0.04
9	DANNY R. SENF AND PEGGY L. SENF, HUSBAND AND WIFE AS SURVIVORSHIP MARITAL PROPERTY	TLE	---	0.05
10	JENNIFER J. MILLER	HE, TLE	0.18	349 SQ.FT
201	TDS TELECOM	RELEASE OF RIGHTS, TEMP. CONST. EASE.		
202	AMERICAN TRANSMISSION COMPANY	RELEASE OF RIGHTS, TEMP. CONST. EASE.		

NOTE: OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO IOWA COUNTY.

REVISION DATE 8-19-21	DATE: MAY 14, 2021	SCALE: FEET 0 50 100	HWY: CTH F	R/W PROJECT NUMBER: 5034-00-03	PLAT SHEET 4.10
GRID FACTOR N/A			COUNTY: IOWA	CONSTRUCTION PROJECT NUMBER: 5034-00-72	PS&E SHEET E

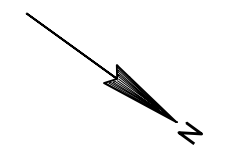
SCHEDULE OF LANDS & INTERESTS REQUIRED

PARCEL NUMBER	OWNER (S)	INTEREST REQUIRED	HE ACRES REQUIRED NEW	TLE ACRES OR S.F. REQ.
1	IRENE BAKER	HE, TLE	0.30	0.06
10	JENNIFER J. MILLER	HE, TLE	0.11	0.40
201	TDS TELECOM	RELEASE OF RIGHTS		
202	AMERICAN TRANSMISSION COMPANY	RELEASE OF RIGHTS, TEMP. CONST. EASE.		

NOTE: OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO IOWA COUNTY.

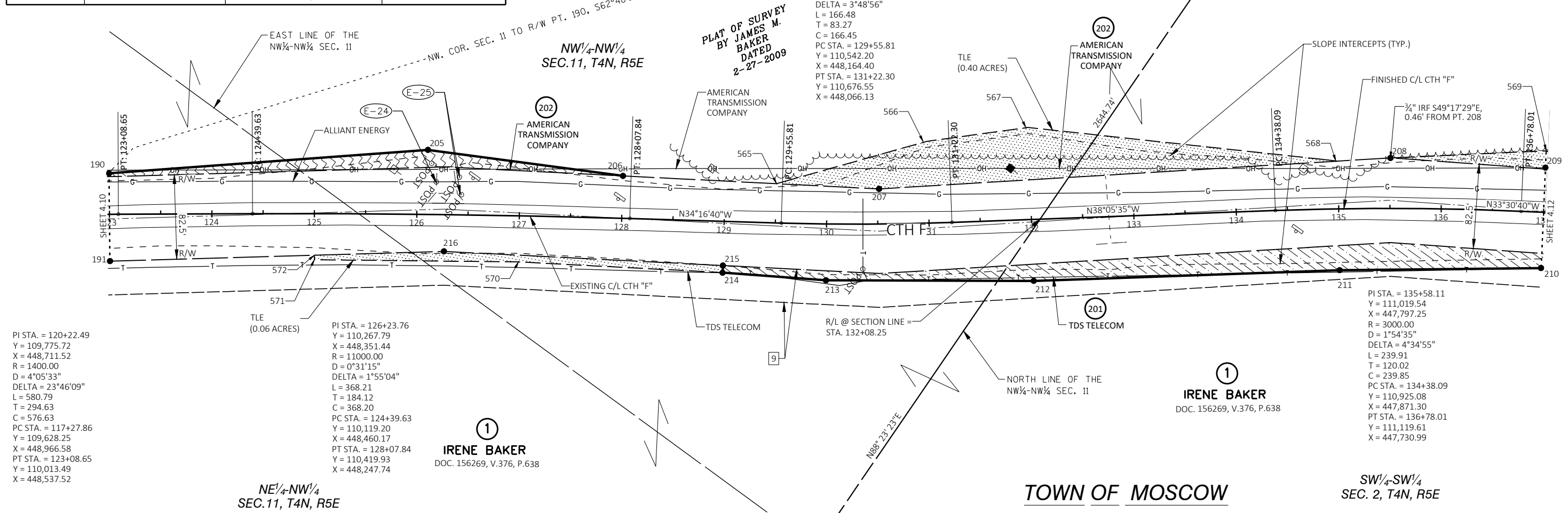
ENCROACHMENT	PROPERTY OWNER	LOCATION (STATION/OFFSET)	ENCROACHMENT TYPE
E-24	JENNIFER MILLER	STA. 126+18, 32' LT.	POST
E-25	JENNIFER MILLER	STA. 126+41, 37' LT.	POSTS

EASEMENT NUMBER	OWNER	RECORDING INFORMATION	LOCATED IN R/W PARCEL #	REMARKS
9	TDS TELECOM	DOC. 227838, V.539, P.22	1	2 ROD EASEMENT



NW CORNER SEC. 11
FD. SLIMLINE HARRISON MONUMENT
Y = 110,718.03'
X = 447,079.48'

SW 1/4-SW 1/4
SEC. 2, T4N, R5E



PI STA. = 120+22.49
Y = 109,775.72
X = 448,711.52
R = 1400.00
D = 4°05'33"
DELTA = 23°46'09"
L = 580.79
T = 294.63
C = 576.63
PC STA. = 117+27.86
Y = 109,628.25
X = 448,966.58
PT STA. = 123+08.65
Y = 110,013.49
X = 448,537.52

PI STA. = 126+23.76
Y = 110,267.79
X = 448,351.44
R = 11000.00
D = 0°31'15"
DELTA = 1°55'04"
L = 368.21
T = 184.12
C = 368.20
PC STA. = 124+39.63
Y = 110,119.20
X = 448,460.17
PT STA. = 128+07.84
Y = 110,419.93
X = 448,247.74

PI STA. = 130+39.09
Y = 110,611.01
X = 448,117.50
R = 2500.00
D = 2°17'31"
DELTA = 3°48'56"
L = 166.48
T = 83.27
C = 166.45
PC STA. = 129+55.81
Y = 110,542.20
X = 448,164.40
PT STA. = 131+22.30
Y = 110,676.55
X = 448,066.13

PI STA. = 135+58.11
Y = 111,019.54
X = 447,797.25
R = 3000.00
D = 1°54'35"
DELTA = 4°34'55"
L = 239.91
T = 120.02
C = 239.85
PC STA. = 134+38.09
Y = 110,925.08
X = 447,871.30
PT STA. = 136+78.01
Y = 111,119.61
X = 447,730.99

PT.#	STATION	OFFSET	Y	X
190	123+00.00	40.00 L	109982.71	448510.52
191	123+00.00	45.76 R	110033.78	448579.41
205	126+10.00	63.00 L	110221.05	448309.22
206	128+00.00	41.47 L	110390.08	448217.91
207	130+51.48	34.37 L	110599.78	448081.38
208	135+50.94	49.36 L	110986.21	447763.40
209	137+00.00	42.22 L	111114.64	447683.64
210	137+00.00	54.67 R	111168.13	447764.43
211	135+00.00	60.00 R	111010.23	447881.58
212	132+00.00	60.00 R	110774.72	448065.41
213	130+00.00	55.00 R	110610.27	448184.09
214	129+00.00	50.00 R	110524.24	448237.15
215	129+00.00	43.17 R	110520.40	448231.51
216	126+26.84	35.92 R	110291.93	448380.25

PT.#	STATION	OFFSET	Y	X
6	135+50.50	49.25 L	110985.91	447763.75

PT.#	STATION	OFFSET	Y	X
565	129+50.00	38.29 L	110515.83	448136.03
566	131+00.00	80.00 L	110610.15	448016.41
567	132+00.00	90.00 L	110682.18	447947.36
568	135+00.00	46.62 L	110946.19	447796.33
569	137+00.00	60.00 L	111104.82	447668.82
570	127+00.00	45.00 R	110356.83	448345.85
571	125+00.00	45.00 R	110194.39	448461.11
572	125+00.00	40.56 R	110191.79	448457.51

N1/4 CORNER SEC. 11
FOUND 1 1/4" IRON ROD
Y = 110,792.35'
X = 449,723.18'

POINT TO POINT	BEARING	DISTANCE
190 TO 205	N40°11'03"W	311.97'
205 TO 206	N28°22'39"W	192.11'
206 TO 207	N33°03'58"W	250.23'
207 TO 208	N39°26'58"W	500.44'
208 TO 209	N31°50'31"W	151.18'
209 TO 210	N56°29'20"E	96.89'
210 TO 211	S36°34'24"E	196.62'
211 TO 212	S37°58'23"E	298.76'
212 TO 213	S35°48'59"E	202.80'
213 TO 214	S31°40'07"E	101.07'
214 TO 215	S55°43'20"W	6.83'
215 TO 216	S33°03'58"E	272.62'
216 TO 191	S37°38'58"E	326.05'
191 TO 190	S53°27'02"W	85.76'

NOTE: EXISTING C/L OF CTH F BASED ON CENTERLINE OF EXISTING PAVEMENT.
EXISTING RIGHT-OF-WAY FOR CTH F BASED ON THE CENTERLINE OF EXISTING PAVEMENT AND WIS. STATUTE 82.31(2).

REVISION DATE 8-19-21	DATE: MAY 14, 2021	SCALE: FEET 0 50 100	HWY: CTH F	R/W PROJECT NUMBER: 5034-00-03	PLAT SHEET 4.11
GRID FACTOR N/A			COUNTY: IOWA	CONSTRUCTION PROJECT NUMBER: 5034-00-72	PS&E SHEET E

SCHEDULE OF LANDS & INTERESTS REQUIRED

PARCEL NUMBER	OWNER (S)	INTEREST REQUIRED	HE ACRES	TLE ACRES
			REQUIRED	OR S.F. REQ.
1	IRENE BAKER	HE, TLE	0.06	0.11
5	RICHARD J. RETRUM, JR. AND SUE M. RETRUM, HUSBAND AND WIFE AS SURVIVORSHIP MARITAL PROPERTY	TLE	---	0.06
10	JENNIFER J. MILLER	TLE	---	0.04
12	STEPHAN A. DIECKHOFF AND LINDA K. DIECKHOFF HIS WIFE	HE, TLE	0.09	0.004
13	RICHARD J. RETRUM, JR. AND SUSAN RETRUM, HUSBAND AND WIFE AS TENANTS IN COMMON	TLE	---	0.01
14	DAVID H. SWENSON (1/5), DIANNA PADLEY (1/5), JAMES SWENSON (1/5), JOHN F. SWENSON (1/5), AND JASON PAUL SWENSON (1/5)	HE, TLE	0.06	0.005
201	TDS TELECOM	RELEASE OF RIGHTS, TEMP. CONST. EASE.		
202	AMERICAN TRANSMISSION COMPANY	RELEASE OF RIGHTS, TEMP. CONST. EASE.		

NOTE: OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO IOWA COUNTY.

ENCROACHMENT TABLE			
ENCROACHMENT	PROPERTY OWNER	LOCATION (STATION/OFFSET)	ENCROACHMENT TYPE
E-26	STEPHEN DIECKHOFF	STA. 142+21 - STA. 142+36, 15' LT. - 34' LT.	RETAINING WALL
E-27	STEPHEN DIECKHOFF	STA. 142+58 - STA. 142+63, 16' LT. - 33.5' LT.	RETAINING WALL
E-28	DIANNA PADLEY, JAMES SWENSON, JOHN SWENSON, JASON SWENSON, DAVID SWENSON	STA. 147+28 - STA. 153+54, 34' RT. - 40' RT.	RETAINING WALL

ERWC17 CURVE DATA PT. 228 TO PT. 229	
ARC LENGTH = 254.08'	RADIUS = 1083.00'
DELTA = 013°26'31"	CHORD BEARING = N22°08'12"W
CHORD LENGTH = 253.50'	

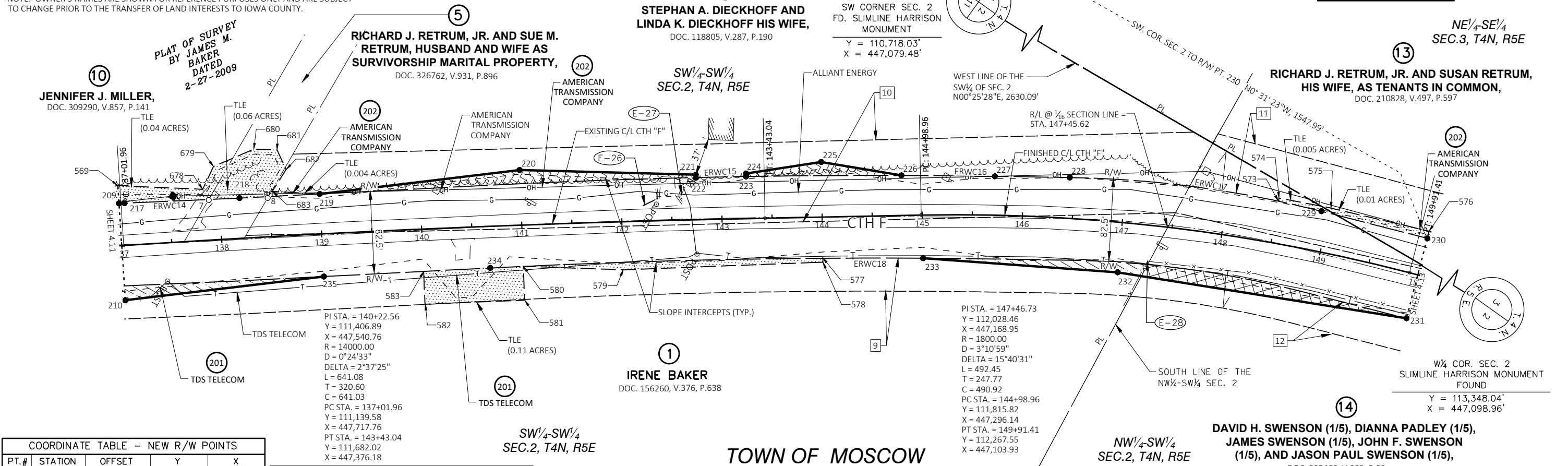
ERWC18 CURVE DATA PT. 233 TO PT. 234	
ARC LENGTH = 432.43'	RADIUS = 7458.75'
DELTA = 003°19'19"	CHORD BEARING = S31°03'33"E
CHORD LENGTH = 432.37'	

RIGHT OF WAY LINE TABLE		
POINT TO POINT	BEARING	DISTANCE
209 TO 217	N31°50'31"W	5.47'
217 TO 218	SEE ERWC14 DATA	
218 TO 219	N32°43'12"W	80.21'
219 TO 220	N36°35'48"W	201.24'
220 TO 221	N28°15'55"W	175.91'
221 TO 222	N58°50'02"E	2.86'
222 TO 223	SEE ERWC15 DATA	
223 TO 224	S59°02'19"W	2.98'
224 TO 225	N38°29'06"W	75.72'
225 TO 226	N20°17'18"W	81.39'
226 TO 227	SEE ERWC16 DATA	
227 TO 228	N28°51'27"W	74.85'
228 TO 229	SEE ERWC17 DATA	
229 TO 230	N15°24'56"W	109.10'
230 TO 231	N74°47'16"E	82.50'
231 TO 232	S20°43'12"E	292.02'
232 TO 233	S25°36'41"E	194.86'
233 TO 234	SEE ERWC18 DATA	
234 TO 235	S32°43'12"E	166.27'
235 TO 210	S36°34'24"E	199.68'
210 TO 209	S56°29'20"W	96.89'

ERWC14 CURVE DATA PT. 217 TO PT. 218	
ARC LENGTH = 114.92'	RADIUS = 7500.00'
DELTA = 000°52'40"	CHORD BEARING = N32°16'52"W
CHORD LENGTH = 114.91'	

ERWC15 CURVE DATA PT. 222 TO PT. 223	
ARC LENGTH = 50.15'	RADIUS = 7541.25'
DELTA = 000°22'52"	CHORD BEARING = N30°56'06"W
CHORD LENGTH = 50.15'	

ERWC16 CURVE DATA PT. 226 TO PT. 227	
ARC LENGTH = 93.28'	RADIUS = 7541.25'
DELTA = 000°42'31"	CHORD BEARING = N29°12'43"W
CHORD LENGTH = 93.28'	



COORDINATE TABLE - NEW R/W POINTS				
PT.#	STATION	OFFSET	Y	X
209	137+00.00	42.22 L	111114.64	447683.64
210	137+00.00	54.67 R	111168.13	447764.43
217	137+05.45	42.06 L	111119.28	447680.76
218	138+20.02	40.09 L	111216.44	447619.39
219	139+00.00	39.89 L	111283.92	447576.03
220	141+00.00	55.00 L	111445.49	447456.05
221	142+75.00	45.00 L	111600.42	447372.75
222	142+75.00	42.14 L	111601.91	447375.20
223	143+25.00	42.02 L	111644.92	447349.42
224	143+25.00	45.00 L	111643.39	447346.87
225	144+00.00	55.00 L	111702.66	447299.74
226	144+80.00	40.03 L	111779.00	447271.52
227	145+71.69	38.80 L	111860.42	447226.00
228	146+44.90	40.70 L	111925.97	447189.87
229	148+93.04	40.01 L	112160.78	447094.35
230	150+00.00	37.65 L	112265.96	447065.35
231	150+00.00	44.85 R	112287.61	447144.96
232	147+00.00	50.00 R	112014.48	447248.28
233	145+00.00	42.98 R	111838.76	447332.51
234	140+66.77	41.56 R	111468.37	447555.58
235	139+00.00	42.61 R	111328.49	447645.45

COORDINATE TABLE - TLE POINTS				
PT.#	STATION	OFFSET	Y	X
569	137+00.00	60.00 L	111104.82	447668.82
573	148+50.00	41.89 L	112118.56	447106.92
574	148+50.00	45.00 L	112117.51	447103.99
575	149+00.00	45.00 L	112165.99	447087.38
576	150+00.00	45.00 L	112264.03	447058.26
577	144+00.00	41.06 R	111751.98	447382.18
578	144+00.00	45.00 R	111754.00	447385.56
579	142+00.00	48.00 R	111584.70	447491.18
580	141+00.00	41.19 R	111496.29	447537.73
581	141+00.00	75.00 R	111514.15	447566.44
582	140+00.00	75.00 R	111429.88	447619.27
583	140+00.00	42.22 R	111412.37	447591.56
678	137+84.00	51.00 L	111180.21	447629.98
679	137+96.00	73.00 L	111178.27	447604.97
680	138+37.00	88.00 L	111204.66	447569.92
681	138+60.00	86.00 L	111225.18	447559.02
682	138+68.00	66.00 L	111242.78	447571.46
683	138+53.00	45.00 L	111241.53	447597.27

COORDINATE TABLE - FOUND SURVEY MONUMENTS				
PT.#	STATION	OFFSET	Y	X
7	137+89.30	39.91 L	111190.73	447636.36
8	138+47.95	38.66 L	111240.72	447605.34

NOTE: EXISTING C/L OF CTH F BASED ON CENTERLINE OF EXISTING PAVEMENT.
EXISTING RIGHT-OF-WAY FOR CTH F BASED ON THE CENTERLINE OF EXISTING PAVEMENT AND WIS. STATUTE 82.31(2).

EASEMENT TABLE				
EASEMENT NUMBER	OWNER	RECORDING INFORMATION	LOCATED IN R/W PARCEL #	REMARKS
9	TDS TELECOM	DOC. 227838, V.539, P.22	1	2 ROD EASEMENT
10	AMERICAN TRANSMISSION COMPANY	DOC. 167439, V.398, P.817	12	80' WIDE EASEMENT
11	AMERICAN TRANSMISSION COMPANY	DOC. 168188, V.401, P.396	13	65' WIDE EASEMENT
12	TDS TELECOM	DOC. 227839, V.539, P.23	14	2 ROD EASEMENT

REVISION DATE 8-19-21	DATE: MAY 14, 2021	SCALE: FEET 0 50 100	HWY: CTH F	R/W PROJECT NUMBER: 5034-00-03	PLAT SHEET 4.12
GRID FACTOR N/A			COUNTY: IOWA	CONSTRUCTION PROJECT NUMBER: 5034-00-72	PS&E SHEET E

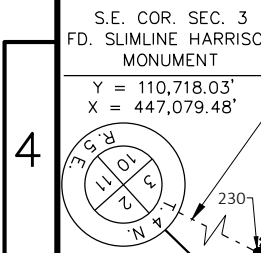
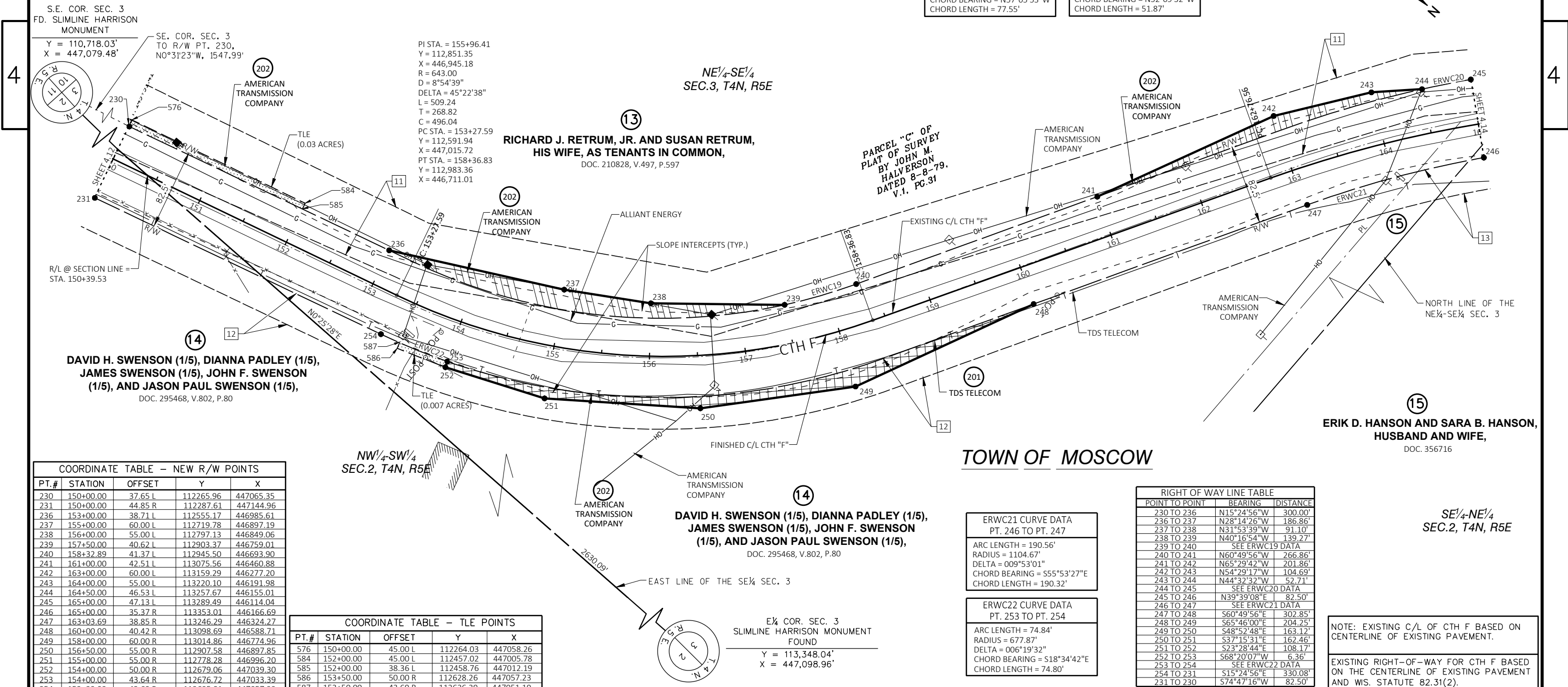
SCHEDULE OF LANDS & INTERESTS REQUIRED

PARCEL NUMBER	OWNER (S)	INTEREST REQUIRED	HE ACRES	TLE ACRES
			REQUIRED NEW	OR S.F. REQ.
13	RICHARD J. RETRUM, JR. AND SUSAN RETRUM, HUSBAND AND WIFE AS TENANTS IN COMMON	HE, TLE	0.21	0.03
14	DAVID H. SWENSON (1/5), DIANNA PADLEY (1/5), JAMES SWENSON (1/5), JOHN F. SWENSON (1/5), AND JASON PAUL SWENSON (1/5)	HE, TLE	0.14	0.007
15	ERIK D. HANSON AND SARA B. HANSON, HUSBAND AND WIFE	HE	---	---
201	TDS TELECOM	RELEASE OF RIGHTS		
202	AMERICAN TRANSMISSION COMPANY	RELEASE OF RIGHTS, TEMP. CONST. EASE.		

NOTE: OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO IOWA COUNTY.

EASEMENT TABLE				
EASEMENT NUMBER	OWNER	RECORDING INFORMATION	LOCATED IN R/W PARCEL #	REMARKS
11	AMERICAN TRANSMISSION COMPANY	DOC. 168188, V.401, P.396	13	65' WIDE EASEMENT
12	TDS TELECOM	DOC. 227839, V.539, P.23	14	2 ROD EASEMENT
13	TDS TELECOM	DOC. 234172, V.553, P.798	15	2 ROD EASEMENT

ERWC19 CURVE DATA PT. 239 TO PT. 240	ERWC20 CURVE DATA PT. 244 TO PT. 245
ARC LENGTH = 77.61' RADIUS = 595.37' DELTA = 007°28'07" CHORD BEARING = N57°05'53"W CHORD LENGTH = 77.55'	ARC LENGTH = 51.88' RADIUS = 1187.17' DELTA = 002°30'13" CHORD BEARING = N52°09'32"W CHORD LENGTH = 51.87'



PI STA. = 155+96.41
Y = 112,851.35
X = 446,945.18
R = 643.00
D = 8°54'39"
DELTA = 45°22'38"
L = 509.24
T = 268.82
C = 496.04
PC STA. = 153+27.59
Y = 112,591.94
X = 447,015.72
PT STA. = 158+36.83
Y = 112,983.36
X = 446,711.01

NE 1/4-SE 1/4 SEC. 3, T4N, R5E

RICHARD J. RETRUM, JR. AND SUSAN RETRUM, HIS WIFE, AS TENANTS IN COMMON, DOC. 210828, V.497, P.597

DAVID H. SWENSON (1/5), DIANNA PADLEY (1/5), JAMES SWENSON (1/5), JOHN F. SWENSON (1/5), AND JASON PAUL SWENSON (1/5), DOC. 295468, V.802, P.80

ERIK D. HANSON AND SARA B. HANSON, HUSBAND AND WIFE, DOC. 356716

DAVID H. SWENSON (1/5), DIANNA PADLEY (1/5), JAMES SWENSON (1/5), JOHN F. SWENSON (1/5), AND JASON PAUL SWENSON (1/5), DOC. 295468, V.802, P.80

COORDINATE TABLE - NEW R/W POINTS				
PT.#	STATION	OFFSET	Y	X
230	150+00.00	37.65 L	112265.96	447065.35
231	150+00.00	44.85 R	112287.61	447144.96
236	153+00.00	38.71 L	112555.17	446985.61
237	155+00.00	60.00 L	112719.78	446897.19
238	156+00.00	55.00 L	112797.13	446849.06
239	157+50.00	40.62 L	112903.37	446759.01
240	158+32.89	41.37 L	112945.50	446693.90
241	161+00.00	42.51 L	113075.56	446460.88
242	163+00.00	60.00 L	113159.29	446277.20
243	164+00.00	55.00 L	113220.10	446191.98
244	164+50.00	46.53 L	113257.67	446155.01
245	165+00.00	47.13 L	113289.49	446114.04
246	165+00.00	35.37 R	113353.01	446166.69
247	163+03.69	38.85 R	113246.29	446324.27
248	160+00.00	40.42 R	113098.69	446588.71
249	158+00.00	60.00 R	113014.86	446774.96
250	156+50.00	55.00 R	112907.58	446897.85
251	155+00.00	55.00 R	112778.28	446996.20
252	154+00.00	50.00 R	112679.06	447039.30
253	154+00.00	43.64 R	112676.72	447033.39
254	153+29.92	43.69 R	112605.81	447057.22

COORDINATE TABLE - TLE POINTS				
PT.#	STATION	OFFSET	Y	X
576	150+00.00	45.00 L	112264.03	447058.26
584	152+00.00	45.00 L	112457.02	447005.78
585	152+00.00	38.36 L	112458.76	447012.19
586	153+50.00	50.00 R	112628.26	447057.23
587	153+50.00	43.69 R	112626.39	447051.19

ERWC21 CURVE DATA PT. 246 TO PT. 247	ERWC22 CURVE DATA PT. 253 TO PT. 254
ARC LENGTH = 190.56' RADIUS = 1104.67' DELTA = 009°53'01" CHORD BEARING = S55°53'27"E CHORD LENGTH = 190.32'	ARC LENGTH = 74.84' RADIUS = 677.87' DELTA = 006°19'32" CHORD BEARING = S18°34'42"E CHORD LENGTH = 74.80'

RIGHT OF WAY LINE TABLE		
POINT TO POINT	BEARING	DISTANCE
230 TO 236	N15°24'56"W	300.00'
236 TO 237	N28°14'26"W	186.86'
237 TO 238	N31°53'39"W	91.10'
238 TO 239	N40°16'54"W	139.27'
239 TO 240	SEE ERWC19 DATA	
240 TO 241	N60°49'56"W	266.86'
241 TO 242	N65°29'42"W	201.86'
242 TO 243	N54°29'17"W	104.69'
243 TO 244	N44°32'32"W	52.71'
244 TO 245	SEE ERWC20 DATA	
245 TO 246	N39°39'08"E	82.50'
246 TO 247	SEE ERWC21 DATA	
247 TO 248	S60°49'56"E	302.85'
248 TO 249	S65°46'00"E	204.25'
249 TO 250	S48°52'48"E	163.12'
250 TO 251	S37°15'31"E	162.46'
251 TO 252	S23°28'44"E	108.17'
252 TO 253	S68°20'07"W	6.36'
253 TO 254	SEE ERWC22 DATA	
254 TO 231	S15°24'56"E	330.08'
231 TO 230	S74°47'16"W	82.50'

NOTE: EXISTING C/L OF CTH F BASED ON CENTERLINE OF EXISTING PAVEMENT.

EXISTING RIGHT-OF-WAY FOR CTH F BASED ON THE CENTERLINE OF EXISTING PAVEMENT AND WIS. STATUTE 82.31(2).

REVISION DATE 8-19-21	DATE: MAY 14, 2021	SCALE: FEET 0 50 100	HWY: CTH F	R/W PROJECT NUMBER: 5034-00-03	PLAT SHEET 4.13
	GRID FACTOR N/A		COUNTY: IOWA	CONSTRUCTION PROJECT NUMBER: 5034-00-72	PS&E SHEET E

COORDINATE TABLE - NEW R/W POINTS				
PT.#	STATION	OFFSET	Y	X
245	165+00.00	47.13 L	113289.49	446114.04
246	165+00.00	35.37 R	113353.01	446166.69
255	166+87.00	47.68 L	113423.65	445974.03
256	168+00.00	55.00 L	113510.18	445894.06
257	169+00.00	55.00 L	113593.79	445834.53
258	169+75.00	50.00 L	113664.08	445797.37
259	403'H+00.00	70.00 R	113651.86	445669.37
260	401'H+25.00	33.52 R	113690.13	445494.75
261	400'H+30.00	33.01 R	113691.62	445399.76
262	400'H+30.00	32.99 L	113757.61	445400.44
263	400'H+30.00	60.00 L	113784.62	445400.72
264	401'H+00.00	60.00 L	113783.90	445470.71
265	402'H+50.00	40.00 L	113762.37	445620.50
266	171+30.00	37.26 L	113817.40	445742.58
267	173+28.65	41.46 L	114015.99	445691.58
268	175+00.00	41.64 L	114185.01	445663.45

COORDINATE TABLE - NEW R/W POINTS				
PT.#	STATION	OFFSET	Y	X
269	176+00.00	55.00 L	114281.49	445633.96
270	177+00.00	55.00 L	114380.15	445617.65
271	178+00.00	41.84 L	114480.96	445614.32
272	179+00.00	41.78 L	114579.63	445598.07
273	179+00.00	45.00 L	114579.11	445594.90
274	179+78.03	52.80 L	114654.82	445574.47
275	179+62.65	43.44 R	114655.34	445671.93
276	178+00.00	55.00 R	114496.76	445709.87
277	177+00.00	45.00 R	114396.46	445716.31
278	177+00.00	40.65 R	114395.75	445712.02
279	174+00.00	40.97 R	114099.83	445761.27
280	172+00.00	70.00 R	113914.40	445825.03
281	169+00.00	60.00 R	113657.84	445930.04
282	167+00.00	45.00 R	113494.79	446034.85

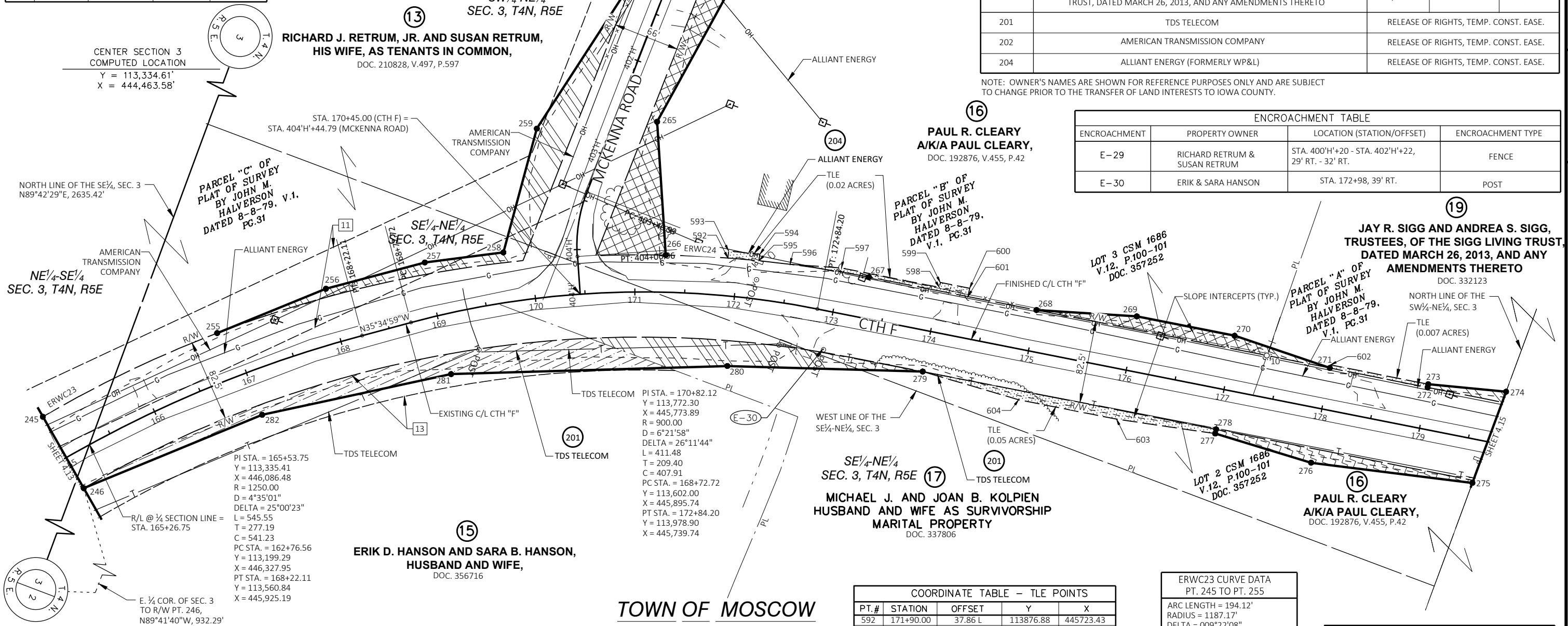
COORDINATE TABLE - FOUND SURVEY MONUMENTS				
PT.#	STATION	OFFSET	Y	X
9	400'H+23.82	33.10 L	113757.79	445394.26
10	177+35.68	41.71 L	114417.52	445624.94

SCHEDULE OF LANDS & INTERESTS REQUIRED

PARCEL NUMBER	OWNER (S)	INTEREST REQUIRED	HE ACRES	TLE
			REQUIRED NEW	ACRES OR S.F. REQ.
13	RICHARD J. RETRUM, JR. AND SUSAN RETRUM, HUSBAND AND WIFE AS TENANTS IN COMMON	HE	0.21	---
15	ERIK D. HANSON AND SARA B. HANSON, HUSBAND AND WIFE	HE	0.23	---
16	PAUL R. CLEARY A/K/A PAUL CLEARY	HE, TLE	0.35	0.07
17	MICHAEL J. AND JOAN B. KOLPIEN HUSBAND AND WIFE AS SURVIVORSHIP MARITAL PROPERTY	HE	0.12	---
19	JAY R. SIGG AND ANDREA S. SIGG, TRUSTEES, OF THE SIGG LIVING TRUST, DATED MARCH 26, 2013, AND ANY AMENDMENTS THERETO	HE, TLE	0.02	0.007
201	TDS TELECOM	RELEASE OF RIGHTS, TEMP. CONST. EASE.		
202	AMERICAN TRANSMISSION COMPANY	RELEASE OF RIGHTS, TEMP. CONST. EASE.		
204	ALLIANT ENERGY (FORMERLY WP&L)	RELEASE OF RIGHTS, TEMP. CONST. EASE.		

NOTE: OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO IOWA COUNTY.

ENCROACHMENT TABLE			
ENCROACHMENT	PROPERTY OWNER	LOCATION (STATION/OFFSET)	ENCROACHMENT TYPE
E-29	RICHARD RETRUM & SUSAN RETRUM	STA. 400'H+20 - STA. 402'H+22, 29' RT. - 32' RT.	FENCE
E-30	ERIK & SARA HANSON	STA. 172+98, 39' RT.	POST



PI STA. = 165+53.75
 Y = 113,335.41
 X = 446,086.48
 R = 1250.00
 D = 4°35'01"
 DELTA = 25°00'23"
 L = 545.55
 T = 277.19
 C = 541.23
 PC STA. = 162+76.56
 Y = 113,199.29
 X = 446,327.95
 PT STA. = 168+22.11
 Y = 113,560.84
 X = 445,925.19

PI STA. = 170+82.12
 Y = 113,772.30
 X = 445,773.89
 R = 900.00
 D = 6°21'58"
 DELTA = 26°11'44"
 L = 411.48
 T = 209.40
 C = 407.91
 PC STA. = 168+72.72
 Y = 113,602.00
 X = 445,895.74
 PT STA. = 172+84.20
 Y = 113,978.90
 X = 445,739.74

EASEMENT TABLE				
EASEMENT NUMBER	OWNER	RECORDING INFORMATION	LOCATED IN R/W PARCEL #	REMARKS
11	AMERICAN TRANSMISSION COMPANY	DOC. 168188, V.401, P.396	13	65' WIDE EASEMENT
13	TDS TELECOM	DOC. 234172, V.553, P.798	15	2 ROD EASEMENT

COORDINATE TABLE - TLE POINTS				
PT.#	STATION	OFFSET	Y	X
592	171+90.00	37.86 L	113876.88	445723.43
593	171+90.00	45.00 L	113874.99	445716.55
594	172+20.00	45.00 L	113905.49	445708.70
595	172+20.00	38.49 L	113907.01	445715.03
596	172+50.00	39.34 L	113937.38	445707.41
597	173+00.00	45.00 L	113987.14	445692.76
598	174+00.00	45.00 L	114085.80	445676.45
599	174+00.00	50.00 L	114084.99	445671.52
600	174+30.00	50.00 L	114114.58	445666.62
601	174+30.00	41.56 L	114115.96	445674.95
602	178+00.00	45.00 L	114480.45	445611.21
603	176+00.00	50.00 R	114298.62	445737.56
604	175+00.00	50.00 R	114199.96	445753.87

ERWC23 CURVE DATA
 PT. 245 TO PT. 255
 ARC LENGTH = 194.12'
 RADIUS = 1187.17'
 DELTA = 009°22'08"
 CHORD BEARING = N46°13'22"W
 CHORD LENGTH = 193.91'

ERWC24 CURVE DATA
 PT. 266 TO PT. 267
 ARC LENGTH = 205.29'
 RADIUS = 1187.17'
 DELTA = 009°54'29"
 CHORD BEARING = N14°24'10"W
 CHORD LENGTH = 205.04'

NOTE: SEE SHEET 4.15 FOR R/W LINE TABLE.

NOTE: EXISTING C/L OF CTH F BASED ON CENTERLINE OF EXISTING PAVEMENT.

EXISTING RIGHT-OF-WAY FOR CTH F BASED ON THE CENTERLINE OF EXISTING PAVEMENT AND WIS. STATUTE 82.31(2).

REVISION DATE 8-19-21	DATE: MAY 14, 2021	SCALE: FEET 0 50 100	HWY: CTH F	R/W PROJECT NUMBER: 5034-00-03	PLAT SHEET 4.14
	GRID FACTOR N/A		COUNTY: IOWA	CONSTRUCTION PROJECT NUMBER: 5034-00-72	PS&E SHEET E

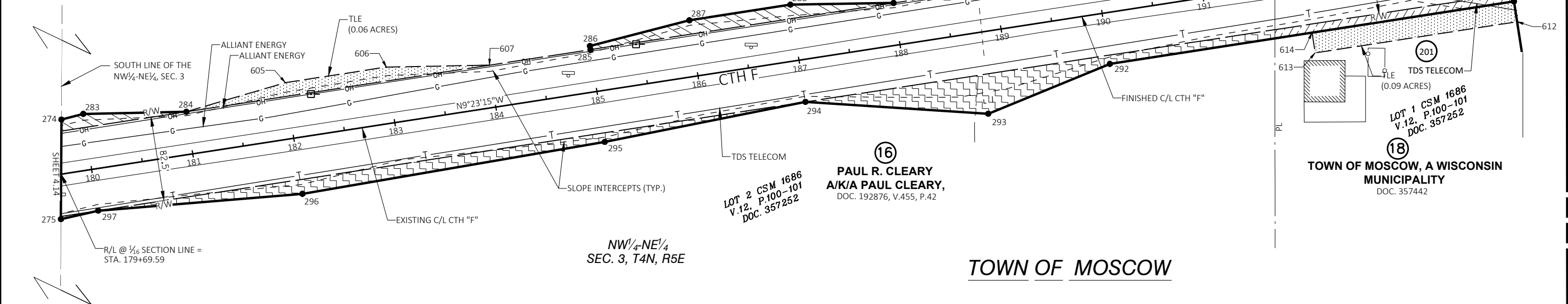
COORDINATE TABLE - NEW R/W POINTS				
PT.#	STATION	OFFSET	Y	X
274	179+78.03	52.80 L	114654.82	445574.47
275	179+62.65	43.44 R	114655.34	445671.93
283	180+00.00	55.00 L	114676.14	445568.72
284	181+00.00	41.65 L	114776.97	445565.58
285	185+00.00	41.40 L	115171.66	445500.58
286	185+00.00	45.00 L	115171.07	445497.03
287	186+00.00	55.00 L	115268.10	445470.85
288	187+00.00	55.00 L	115366.76	445454.54
289	188+00.00	41.21 L	115467.67	445451.83
290	194+00.00	40.83 L	116059.70	445354.34
291	194+00.00	50.00 R	116074.51	445443.96
292	190+00.00	50.00 R	115679.87	445509.20
293	188+75.00	80.00 R	115561.44	445559.19
294	187+00.00	41.23 R	115382.46	445549.48
295	185+00.00	50.00 R	115186.57	445590.76
296	182+00.00	55.00 R	114891.40	445644.62
297	180+00.00	40.78 R	114691.76	445663.22

COORDINATE TABLE - TLE POINTS				
PT.#	STATION	OFFSET	Y	X
605	182+00.00	55.00 L	114873.46	445536.10
606	183+00.00	55.00 L	114972.12	445519.78
607	184+00.00	41.46 L	115072.99	445516.83
608	188+25.00	41.19 L	115492.34	445447.77
609	188+25.00	57.00 L	115489.76	445432.18
610	189+50.00	57.00 L	115613.09	445411.79
611	189+50.00	41.12 L	115615.68	445427.46
612	194+00.00	70.00 R	116077.78	445463.69
613	192+00.00	70.00 R	115880.45	445496.31
614	192+00.00	50.00 R	115877.19	445476.58

SCHEDULE OF LANDS & INTERESTS REQUIRED				
PARCEL NUMBER	OWNER (S)	INTEREST REQUIRED	HE ACRES REQUIRED NEW	TLE ACRES OR S.F. REQ.
16	PAUL R. CLEARY A/K/A PAUL CLEARY	HE	0.31	---
18	TOWN OF MOSCOW, A WISCONSIN MUNICIPALITY	HE, TLE	0.05	0.09
19	JAY R. SIGG AND ANDREA S. SIGG, TRUSTEES, OF THE SIGG LIVING TRUST, DATED MARCH 26, 2013, AND ANY AMENDMENTS THERETO	HE, TLE	0.09	0.11
201	TDS TELECOM	RELEASE OF RIGHTS, TEMP. CONST. EASE.		

NOTE: OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO IOWA COUNTY.

RIGHT OF WAY LINE TABLE		
POINT TO POINT	BEARING	DISTANCE
290 TO 291	N80°36'45"E	90.83'
291 TO 292	S09°23'15"E	400.00'
292 TO 293	S22°53'00"E	128.55'
293 TO 294	S03°06'19"W	179.24'
294 TO 295	S11°53'58"E	200.19'
295 TO 296	S10°20'32"E	300.04'
296 TO 297	S05°19'19"E	200.51'
297 TO 275	S13°27'11"E	37.45'
275 TO 274	S89°41'40"W	97.46'
274 TO 283	N15°05'53"W	22.08'
283 TO 284	N01°47'04"W	100.88'
284 TO 285	N09°21'05"W	400.00'
285 TO 286	S80°35'55"W	3.60'
286 TO 287	N15°05'53"W	100.50'
287 TO 288	N09°23'15"W	100.00'
288 TO 289	N01°32'10"W	100.94'
289 TO 290	N09°21'05"W	600.00'



SHEET 4.14 RIGHT OF WAY LINE TABLE		
POINT TO POINT	BEARING	DISTANCE
245 TO 255	SEE ERWC23 DATA	
255 TO 256	N42°44'43"W	117.83'
256 TO 257	N35°27'08"W	102.63'
257 TO 258	N27°51'25"W	79.51'
258 TO 259	S84°32'49"W	128.58'
259 TO 260	N77°38'23"W	178.77'
260 TO 261	N89°06'13"W	95.00'
261 TO 262	N00°35'11"E	66.00'
262 TO 263	N00°35'11"E	27.01'
263 TO 264	S89°24'49"E	70.00'
264 TO 265	S81°49'08"E	151.33'
265 TO 266	N65°44'17"E	133.91'
266 TO 267	SEE ERWC24 DATA	
267 TO 268	N09°26'56"W	171.35'
268 TO 269	N16°59'51"W	100.89'
269 TO 270	N09°23'15"W	100.00'

SHEET 4.14 RIGHT OF WAY LINE TABLE		
POINT TO POINT	BEARING	DISTANCE
270 TO 271	N01°53'27"W	100.86'
271 TO 272	N09°21'05"W	100.00'
272 TO 273	S80°40'09"W	3.22'
273 TO 274	N15°05'53"W	78.42'
274 TO 275	N89°41'40"E	97.46'
275 TO 276	S13°27'11"E	163.06'
276 TO 277	S03°40'37"E	100.50'
277 TO 278	S80°36'45"W	4.35'
278 TO 279	S09°26'56"E	300.00'
279 TO 280	S18°58'35"E	196.08'
280 TO 281	S22°15'32"E	277.22'
281 TO 282	S32°43'57"E	193.83'
282 TO 246	S42°55'20"E	193.60'
246 TO 245	S39°39'08"W	82.50'

NOTE: EXISTING C/L OF CTH F BASED ON CENTERLINE OF EXISTING PAVEMENT.

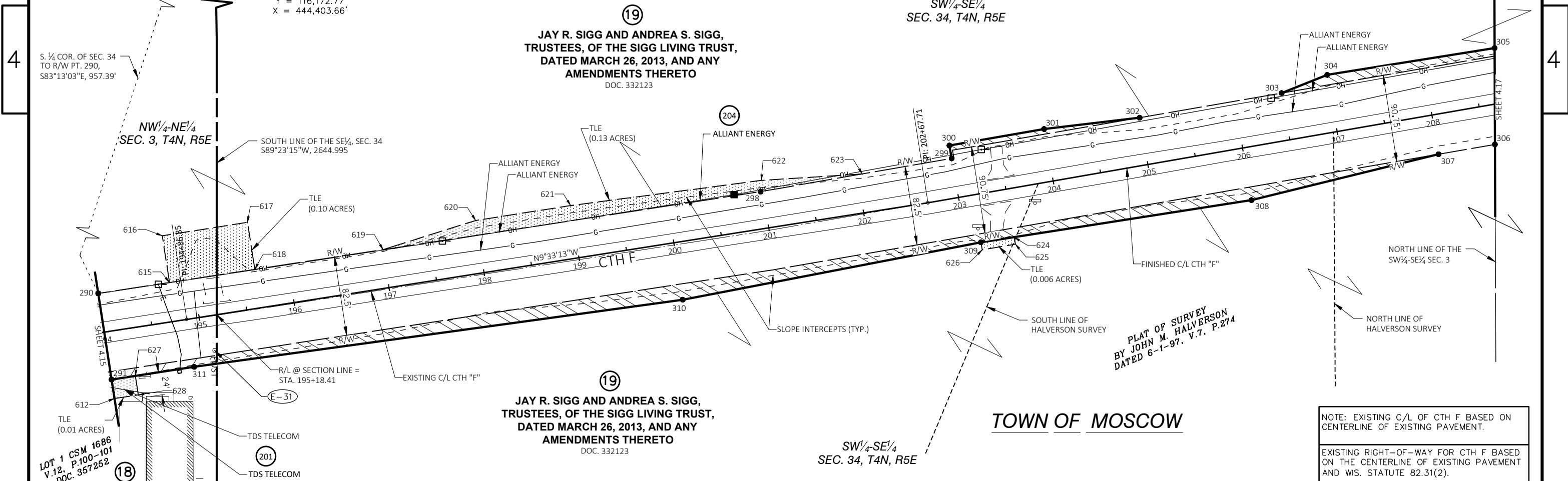
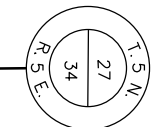
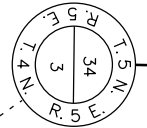
EXISTING RIGHT-OF-WAY FOR CTH F BASED ON THE CENTERLINE OF EXISTING PAVEMENT AND WIS. STATUTE 82.31(2).

REVISION DATE 8-19-21	DATE: MAY 14, 2021	SCALE, FEET 0 50 100	HWY: CTH F	R/W PROJECT NUMBER: 5034-00-03	PLAT SHEET 4.15
	GRID FACTOR N/A		COUNTY: IOWA	CONSTRUCTION PROJECT NUMBER: 5034-00-72	PS&E SHEET E

ENCROACHMENT TABLE			
ENCROACHMENT	PROPERTY OWNER	LOCATION (STATION/OFFSET)	ENCROACHMENT TYPE
E-31	TOWN OF MOSCOW	STA. 195+11, 36.5' RT.	POST



N¼ SECTION 34
0.75" IRON ROD FOUND
Y = 121,499.26'
X = 444,396.19'



PT.#	STATION	OFFSET	Y	X
290	194+00.00	40.83 L	116059.70	445354.34
291	194+00.00	50.00 R	116074.51	445443.96
298	200+97.48	38.62 L	116747.80	445241.03
299	203+00.00	41.33 L	116946.31	445204.35
300	203+00.00	55.00 L	116943.82	445190.90
301	204+00.00	55.00 L	117042.15	445172.71
302	205+00.00	49.53 L	117141.47	445159.89
303	206+50.00	49.49 L	117288.98	445132.63
304	207+00.00	60.00 L	117336.23	445113.20
305	208+76.36	57.66 L	117510.07	445083.42
306	208+59.49	41.31 R	117511.49	445183.80
307	208+00.00	41.30 R	117452.99	445194.61
308	206+00.00	55.00 R	117258.82	445244.48
309	203+15.00	50.00 R	116977.67	445291.42
310	200+00.00	60.00 R	116668.04	445354.46
311	194+86.85	50.00 R	116160.20	445429.79

PT.#	STATION	OFFSET	Y	X
612	194+00.00	70.00 R	116077.78	445463.69
615	194+75.00	40.78 L	116133.70	445342.16
616	194+75.00	90.00 L	116125.67	445293.60
617	195+65.00	90.00 L	116214.17	445278.74
618	195+65.00	40.50 L	116222.39	445327.55
619	197+00.00	40.02 L	116355.60	445305.61
620	198+00.00	55.00 L	116451.72	445274.25
621	199+00.00	55.00 L	116550.34	445257.65
622	201+00.00	50.00 L	116748.39	445229.39
623	202+05.00	40.34 L	116853.54	445221.49
624	203+50.00	50.61 R	117012.20	445285.66
625	203+50.00	58.00 R	117013.54	445292.92
626	203+15.00	58.00 R	116979.13	445299.29
627	194+25.00	50.00 R	116099.18	445439.88
628	194+25.00	70.00 R	116102.44	445459.61

POINT TO POINT	BEARING	DISTANCE
290 TO 298	N09°21'05"W	697.37'
298 TO 299	N10°28'10"W	201.87'
299 TO 300	S79°30'59"W	13.67'
300 TO 301	N10°29'01"W	100.00'
301 TO 302	N07°21'07"W	100.15'
302 TO 303	N10°28'10"W	150.00'
303 TO 304	N22°21'10"W	51.09'
304 TO 305	N09°43'23"W	176.38'
305 TO 306	N89°11'29"E	100.40'
306 TO 307	S10°28'10"E	59.49'
307 TO 308	S14°24'13"E	200.47'
308 TO 309	S09°28'43"E	285.04'
309 TO 310	S11°30'28"E	315.99'
310 TO 311	S08°26'15"E	513.39'
311 TO 291	S09°23'15"E	86.85'
291 TO 290	S80°36'45"W	90.83'

PARCEL NUMBER	OWNER (S)	INTEREST REQUIRED	HE ACRES	TLE
			REQUIRED	ACRES OR S.F. REQ.
18	TOWN OF MOSCOW, A WISCONSIN MUNICIPALITY	HE, TLE	0.02	0.01
19	JAY R. SIGG AND ANDREA S. SIGG, TRUSTEES, OF THE SIGG LIVING TRUST, DATED MARCH 26, 2013, AND ANY AMENDMENTS THERETO	HE, TLE	0.40	0.24
201	TDS TELECOM		TEMP. CONST. EASE.	
204	ALLIANT ENERGY		RELEASE OF RIGHTS	

NOTE: OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO IOWA COUNTY.

REVISION DATE 8-19-21	DATE: MAY 14, 2021	SCALE: FEET 0 50 100	HWY: CTH F	R/W PROJECT NUMBER: 5034-00-03	PLAT SHEET 4.16
	GRID FACTOR N/A		COUNTY: IOWA	CONSTRUCTION PROJECT NUMBER: 5034-00-72	PS&E SHEET E

SCHEDULE OF LANDS & INTERESTS REQUIRED

PARCEL NUMBER	OWNER (S)	INTEREST REQUIRED	HE ACRES REQUIRED NEW	TLE ACRES OR S.F. REQ.
19	JAY R. SIGG AND ANDREA S. SIGG, TRUSTEES, OF THE SIGG LIVING TRUST, DATED MARCH 26, 2013, AND ANY AMENDMENTS THERETO	HE, TLE	0.57	0.53
204	ALLIANT ENERGY	RELEASE OF RIGHTS		

NOTE: OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO IOWA COUNTY.

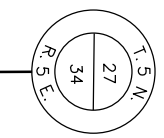
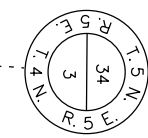
PT.#	STATION	OFFSET	Y	X
11	212+80.45	29.22 L	117915.82	445043.77



ENCROACHMENT	PROPERTY OWNER	LOCATION (STATION/OFFSET)	ENCROACHMENT TYPE
E-32	SIGG LIVING TRUST	STA. 212+65 - STA. 212+84, 29.5' LT. - 42' LT.	FENCE
E-33	SIGG LIVING TRUST	STA. 213+11 - STA. 214+01, 36.5' RT. - 41' RT.	FENCE
E-34	SIGG LIVING TRUST	STA. 213+94 - STA. 214+08, 38' RT. - 41' RT.	RAMP

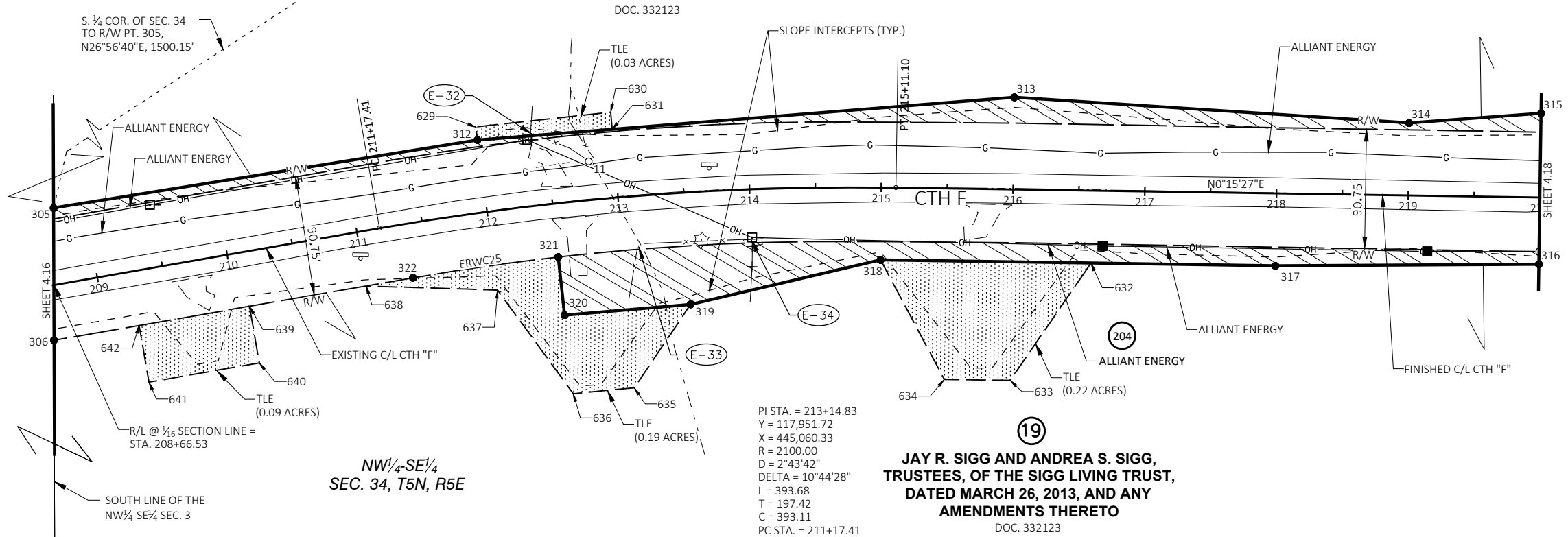
S1/4 SECTION 34
0.75" IRON ROD FOUND
Y = 116,172.77'
X = 444,403.66'

N 1/4 SECTION 34
0.75" IRON ROD FOUND
Y = 121,499.26'
X = 444,396.19'



19
JAY R. SIGG AND ANDREA S. SIGG,
TRUSTEES, OF THE SIGG LIVING TRUST,
DATED MARCH 26, 2013, AND ANY
AMENDMENTS THERETO
DOC. 332123

NW 1/4-SE 1/4
SEC. 34, T5N, R5E



PI STA. = 213+14.83
Y = 117,951.72
X = 445,060.33
R = 2100.00
D = 2°43'42"
DELTA = 10°44'28"
L = 393.68
T = 197.42
C = 393.11
PC STA. = 211+17.41
Y = 117,757.59
X = 445,096.25
PT STA. = 215+11.10
Y = 118,149.14
X = 445,061.22

19
JAY R. SIGG AND ANDREA S. SIGG,
TRUSTEES, OF THE SIGG LIVING TRUST,
DATED MARCH 26, 2013, AND ANY
AMENDMENTS THERETO
DOC. 332123

TOWN OF MOSCOW

PT.#	STATION	OFFSET	Y	X
305	208+76.36	57.66 L	117510.07	445083.42
306	208+59.49	41.31 R	117511.49	445183.80
312	212+00.00	55.00 L	117831.20	445028.39
313	216+00.00	70.00 L	118238.35	444991.62
314	219+00.00	55.00 L	118538.28	445007.96
315	220+00.00	63.91 L	118638.32	444999.50
316	220+00.00	50.79 R	118637.81	445114.21
317	218+00.00	55.00 R	118437.79	445117.51
318	215+00.00	55.00 R	118138.08	445116.20
319	213+50.00	85.00 R	117994.30	445151.45
320	212+50.00	85.00 R	117898.80	445160.65
321	212+50.00	40.63 R	117893.50	445116.59
322	211+36.37	41.30 R	117783.39	445133.56

PT.#	STATION	OFFSET	Y	X
629	212+00.00	65.00 L	117829.77	445018.49
630	213+00.00	65.00 L	117932.12	445006.17
631	213+00.00	51.95 L	117933.37	445019.16
632	216+60.00	55.01 R	118297.79	445116.90
633	216+00.00	145.00 R	118237.39	445206.61
634	215+50.00	145.00 R	118187.39	445206.39
635	213+00.00	145.00 R	117952.25	445215.20
636	212+50.00	145.00 R	117905.98	445220.22
637	212+00.00	60.00 R	117847.67	445142.21
638	211+00.00	41.37 R	117748.00	445140.10
639	210+10.00	41.35 R	117659.50	445156.45
640	210+10.00	85.00 R	117667.44	445199.38
641	209+25.00	85.00 R	117583.86	445214.84
642	209+25.00	41.33 R	117575.91	445171.90

POINT TO POINT	BEARING	DISTANCE
305 TO 312	N09°43'23"W	325.81'
312 TO 313	N05°09'40"W	408.81'
313 TO 314	N03°07'12"E	300.37'
314 TO 315	N04°50'10"W	100.40'
315 TO 316	S89°44'33"E	114.71'
316 TO 317	S00°56'49"E	200.04'
317 TO 318	S00°15'07"W	299.71'
318 TO 319	S13°46'34"E	148.04'
319 TO 320	S05°30'07"E	95.94'
320 TO 321	S83°08'01"W	44.37'
321 TO 322	SEE ERWC25 DATA	
322 TO 306	S10°28'10"E	276.50'
306 TO 305	S89°11'29"W	100.40'

ERWC25 CURVE DATA
PT. 321 TO PT. 322
ARC LENGTH = 111.42'
RADIUS = 1868.61'
DELTA = 003°24'59"
CHORD BEARING = S08°45'40"E
CHORD LENGTH = 111.40'

NOTE: EXISTING C/L OF CTH F BASED ON CENTERLINE OF EXISTING PAVEMENT.

EXISTING RIGHT-OF-WAY FOR CTH F BASED ON THE CENTERLINE OF EXISTING PAVEMENT AND WIS. STATUTE 82.31(2).

REVISION DATE 8-19-21	DATE: MAY 14, 2021	SCALE: FEET 0 50 100	HWY: CTH F	R/W PROJECT NUMBER: 5034-00-03	PLAT SHEET 4.17
	GRID FACTOR N/A		COUNTY: IOWA	CONSTRUCTION PROJECT NUMBER: 5034-00-72	PS&E SHEET E

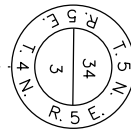
SCHEDULE OF LANDS & INTERESTS REQUIRED

PARCEL NUMBER	OWNER (S)	INTEREST REQUIRED	HE ACRES REQUIRED NEW	TLE ACRES OR S.F. REQ.
19	JAY R. SIGG AND ANDREA S. SIGG, TRUSTEES, OF THE SIGG LIVING TRUST, DATED MARCH 26, 2013, AND ANY AMENDMENTS THERETO	HE, TLE	0.15	0.19
20	RUNDEN PROPERTIES, LLC	HE, TLE	0.55	0.09
203	FRONTIER COMMUNICATIONS	RELEASE OF RIGHTS		
204	ALLIANT ENERGY	RELEASE OF RIGHTS		

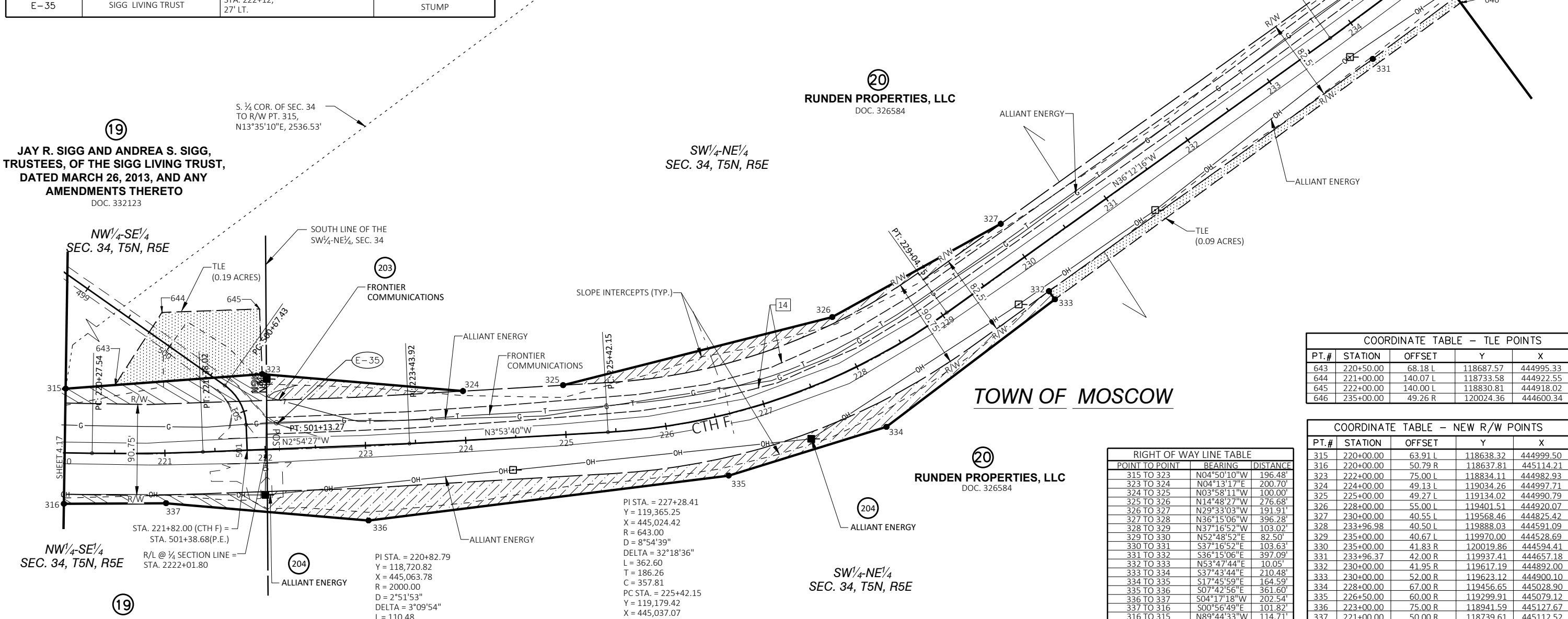
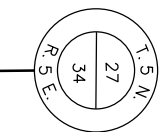
NOTE: OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO IOWA COUNTY.

ENCROACHMENT TABLE			
ENCROACHMENT	PROPERTY OWNER	LOCATION (STATION/OFFSET)	ENCROACHMENT TYPE
E-35	SIGG LIVING TRUST	STA. 222+12, 27' LT.	STUMP

S1/4 SECTION 34
0.75" IRON ROD FOUND
Y = 116,172.77'
X = 444,403.66'



N1/4 SECTION 34
0.75" IRON ROD FOUND
Y = 121,499.26'
X = 444,396.19'



COORDINATE TABLE - TLE POINTS				
PT.#	STATION	OFFSET	Y	X
643	220+50.00	68.18 L	118687.57	444995.33
644	221+00.00	140.07 L	118733.58	444922.55
645	222+00.00	140.00 L	118830.81	444918.02
646	235+00.00	49.26 R	120024.36	444600.34

COORDINATE TABLE - NEW R/W POINTS				
PT.#	STATION	OFFSET	Y	X
315	220+00.00	63.91 L	118638.32	444999.50
316	220+00.00	50.79 R	118637.81	445114.21
323	222+00.00	75.00 L	118834.11	444982.93
324	224+00.00	49.13 L	119034.26	444997.71
325	225+00.00	49.27 L	119134.02	444990.79
326	228+00.00	55.00 L	119401.51	444920.07
327	230+00.00	40.55 L	119568.46	444825.42
328	233+96.98	40.50 L	119888.03	444591.09
329	235+00.00	40.67 L	119970.00	444528.69
330	235+00.00	41.83 R	120019.86	444594.41
331	233+96.37	42.00 R	119937.41	444657.18
332	230+00.00	41.95 R	119617.19	444892.00
333	230+00.00	52.00 R	119623.12	444900.10
334	228+00.00	67.00 R	119456.65	445028.90
335	226+50.00	60.00 R	119299.91	445079.12
336	223+00.00	75.00 R	118941.59	445127.67
337	221+00.00	50.00 R	118739.61	445112.52

RIGHT OF WAY LINE TABLE		
POINT TO POINT	BEARING	DISTANCE
315 TO 323	N04°50'10"W	196.48'
323 TO 324	N04°13'17"E	200.70'
324 TO 325	N03°58'11"W	100.00'
325 TO 326	N14°48'27"W	276.68'
326 TO 327	N29°33'03"W	191.91'
327 TO 328	N36°15'06"W	396.28'
328 TO 329	N37°16'52"W	103.02'
329 TO 330	N52°48'52"E	82.50'
330 TO 331	S37°16'52"E	103.63'
331 TO 332	S36°15'06"E	397.09'
332 TO 333	N53°47'44"E	10.05'
333 TO 334	S37°43'44"E	210.48'
334 TO 335	S17°45'59"E	164.59'
335 TO 336	S07°42'56"E	361.60'
336 TO 337	S04°17'18"W	202.54'
337 TO 316	S00°56'49"E	101.82'
316 TO 315	N89°44'33"W	114.71'

EASEMENT TABLE				
EASEMENT NUMBER	OWNER	RECORDING INFORMATION	LOCATED IN R/W PARCEL #	REMARKS
14	FRONTIER COMMUNICATIONS (FORMERLY GTE MTO INC)	DOC. 167086, V.399, P.208	20	1 ROD EASEMENT

NOTE: EXISTING C/L OF CTH F BASED ON CENTERLINE OF EXISTING PAVEMENT.
EXISTING RIGHT-OF-WAY FOR CTH F BASED ON THE CENTERLINE OF EXISTING PAVEMENT AND WIS. STATUTE 82.31(2).

REVISION DATE 8-19-21	DATE: MAY 14, 2021	SCALE: FEET 0 50 100	HWY: CTH F	R/W PROJECT NUMBER: 5034-00-03	PLAT SHEET 4.18
	GRID FACTOR N/A		COUNTY: IOWA	CONSTRUCTION PROJECT NUMBER: 5034-00-72	PS&E SHEET E

SCHEDULE OF LANDS & INTERESTS REQUIRED

PARCEL NUMBER	OWNER (S)	INTEREST REQUIRED	HE ACRES REQUIRED		TLE ACRES OR S.F. REQ.
			NEW		
19	JAY R. SIGG AND ANDREA S. SIGG, TRUSTEES, OF THE SIGG LIVING TRUST, DATED MARCH 26, 2013, AND ANY AMENDMENTS THERETO	HE, TLE	0.67	---	0.08
20	RUNDEN PROPERTIES, LLC	TLE	---	---	0.27
21	CHARLENE BURKELAND	FEE, TLE	---	0.10	0.01
22	CHARLES L. HARTLEY, JR. AND TRACEY L. HARTLEY, HUSBAND AND WIFE, AS SURVIVORSHIP MARITAL PROPERTY	HE	0.53	---	---
23	JAMES MCGHEE AND MALA MCGHEE, HUSBAND AND WIFE, AS SURVIVORSHIP MARITAL PROPERTY	HE, TLE	0.19	---	0.03
203	FRONTIER COMMUNICATIONS	RELEASE OF RIGHTS, TEMP. CONST. EASE.			
204	ALLIANT ENERGY	RELEASE OF RIGHTS, TEMP. CONST. EASE.			

NOTE: OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO IOWA COUNTY.

COORDINATE TABLE - TLE POINTS

PT.#	STATION	OFFSET	Y	X
646	235+00.00	49.26 R	120024.36	444600.34
647	236+00.00	127.00 L	119997.49	444399.47
648	247+00.00	47.50 R	120950.58	443833.71
649	246+00.00	45.00 R	120872.56	443896.31
650	246+00.00	39.23 R	120868.84	443891.91
651	237+00.00	60.00 R	120190.55	444487.05
652	236+00.00	60.00 R	120110.51	444548.45
653	236+00.00	50.00 R	120104.47	444540.48
674	402'+27.00	33.78 R	120155.19	443657.58
675	402'+27.00	46.50 R	120142.47	443658.06
676	401'+75.00	46.50 R	120139.90	443607.33
677	401'+75.00	34.29 R	120152.09	443606.55

COORDINATE TABLE - NEW R/W POINTS

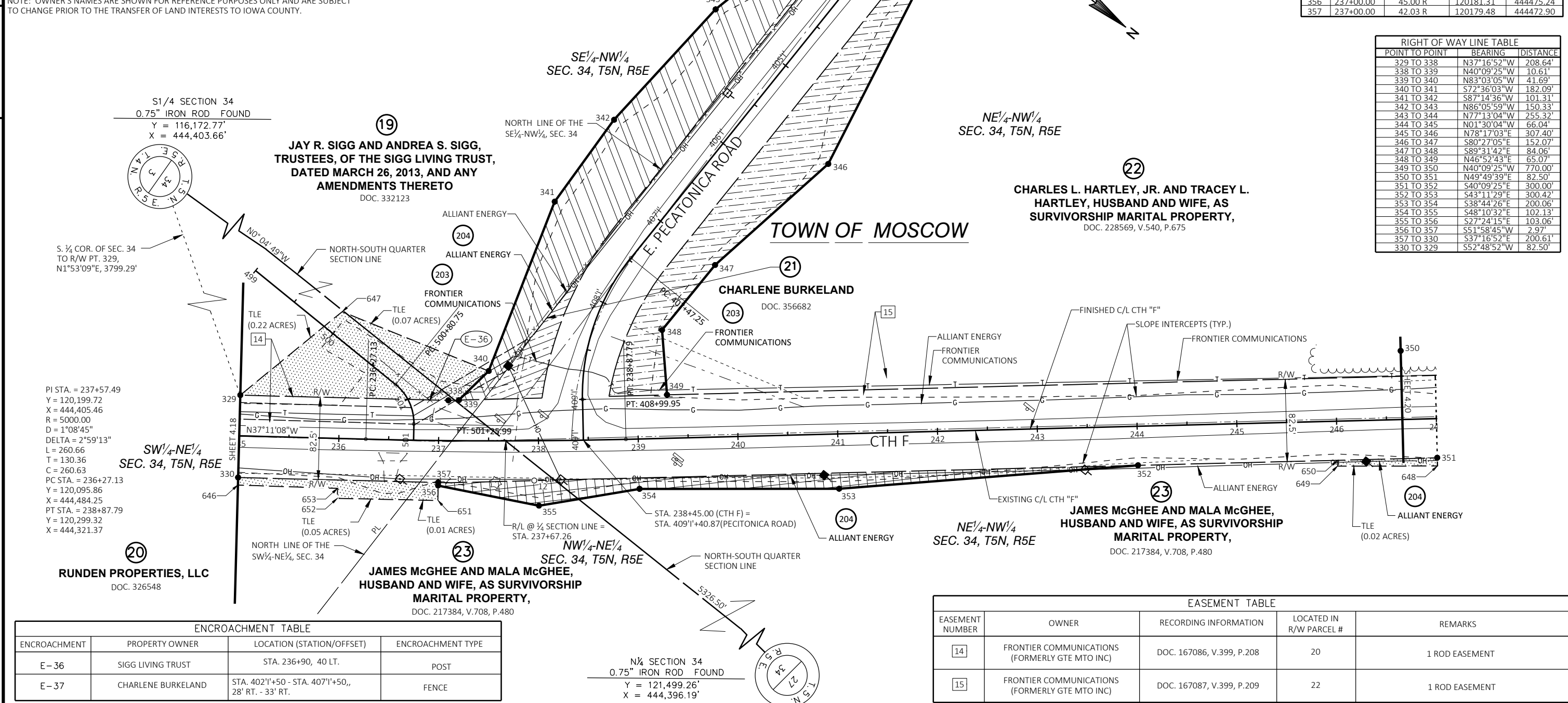
PT.#	STATION	OFFSET	Y	X
329	235+00.00	40.67 L	119970.00	444528.69
330	235+00.00	41.83 R	120019.86	444594.41
338	237+09.31	40.35 L	120136.01	444402.31
339	237+20.00	40.71 L	120144.12	444395.46
340	237+50.00	70.00 L	120149.16	444354.08
341	407'+50.00	95.00 R	120094.72	444180.33
342	406'+50.00	100.00 R	120089.84	444079.13
343	405'+50.00	90.00 R	120100.07	443929.15
344	402'+50.00	33.13 R	120156.56	443680.16
345	402'+50.00	32.91 L	120222.57	443678.43
346	405'+50.00	95.00 L	120284.99	443979.43
347	407'+50.00	70.00 L	120259.77	444129.39
348	408'+50.00	65.00 L	120259.08	444213.45
349	239+30.00	43.45 L	120303.55	444260.94
350	247+00.00	43.24 L	120892.05	443764.38
351	247+00.00	39.26 R	120945.27	443827.42
352	244+00.00	39.18 R	120715.98	444020.88
353	241+00.00	55.00 R	120496.96	444226.50
354	239+00.00	50.00 R	120340.91	444351.70
355	238+00.00	65.00 R	120272.80	444427.80
356	237+00.00	45.00 R	120181.31	444475.24
357	237+00.00	42.03 R	120179.48	444472.90

COORDINATE TABLE - FOUND SURVEY MONUMENTS

PT.#	STATION	OFFSET	Y	X
12	237+95.52	39.95 R	120253.48	444411.23

RIGHT OF WAY LINE TABLE

POINT TO POINT	BEARING	DISTANCE
329 TO 338	N37°16'52"W	208.64'
338 TO 339	N40°09'25"W	10.61'
339 TO 340	N83°03'05"W	41.69'
340 TO 341	S72°36'03"W	182.09'
341 TO 342	S87°14'36"W	101.31'
342 TO 343	N86°05'59"W	150.33'
343 TO 344	N77°13'04"W	255.32'
344 TO 345	N01°30'04"W	66.04'
345 TO 346	N78°17'03"E	307.40'
346 TO 347	S80°27'05"E	152.07'
347 TO 348	S89°31'42"E	84.06'
348 TO 349	N46°52'43"E	65.07'
349 TO 350	N40°09'25"W	770.00'
350 TO 351	N49°49'39"E	82.50'
351 TO 352	S40°09'25"E	300.00'
352 TO 353	S43°11'29"E	300.42'
353 TO 354	S38°44'26"E	200.06'
354 TO 355	S48°10'32"E	102.13'
355 TO 356	S27°24'15"E	103.06'
356 TO 357	S51°58'45"W	2.97'
357 TO 330	S37°16'52"E	200.61'
330 TO 329	S52°48'52"W	82.50'



ENCROACHMENT TABLE

ENCROACHMENT	PROPERTY OWNER	LOCATION (STATION/OFFSET)	ENCROACHMENT TYPE
E-36	SIGG LIVING TRUST	STA. 236+90, 40 LT.	POST
E-37	CHARLENE BURKELAND	STA. 402'+50 - STA. 407'+50,, 28' RT. - 33' RT.	FENCE

EASEMENT TABLE

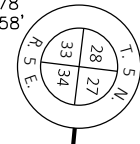
EASEMENT NUMBER	OWNER	RECORDING INFORMATION	LOCATED IN R/W PARCEL #	REMARKS
14	FRONTIER COMMUNICATIONS (FORMERLY GTE MTO INC)	DOC. 167086, V.399, P.208	20	1 ROD EASEMENT
15	FRONTIER COMMUNICATIONS (FORMERLY GTE MTO INC)	DOC. 167087, V.399, P.209	22	1 ROD EASEMENT

REVISION DATE 8-19-21	DATE: MAY 14, 2021	SCALE: FEET 0 50 100	HWY: CTH F	R/W PROJECT NUMBER: 5034-00-03	PLAT SHEET 4.19
	GRID FACTOR N/A		COUNTY: IOWA	CONSTRUCTION PROJECT NUMBER: 5034-00-72	PS&E SHEET E

SCHEDULE OF LANDS & INTERESTS REQUIRED

PARCEL NUMBER	OWNER (S)	INTEREST REQUIRED	HE ACRES REQUIRED NEW	TLE ACRES OR S.F. REQ.
22	CHARLES L. HARTLEY, JR. AND TRACEY L. HARTLEY, HUSBAND AND WIFE, AS SURVIVORSHIP MARITAL PROPERTY	TLE	---	0.01
23	JAMES MCGHEE AND MALA MCGHEE, HUSBAND AND WIFE, AS SURVIVORSHIP MARITAL PROPERTY	TLE	---	0.04
24	ERIC M. NEAL, A SINGLE PERSON	HE, TLE	0.08	0.15
25	JAMES R. THOMPSON	HE, TLE	0.21	0.05
26	TRISTATE DIRECTIONAL DRILLING, LLC A WISCONSIN LIMITED LIABILITY COMPANY	HE, TLE	0.01	0.08
203	FRONTIER COMMUNICATIONS	RELEASE OF RIGHTS, TEMP. CONST. EASE.		
204	ALLIANT ENERGY	RELEASE OF RIGHTS		

SW CORNER SEC. 27
0.75" IRON ROD FOUND
Y = 121,441.78'
X = 441,756.58'



POINT TO POINT	BEARING	DISTANCE
351 TO 350	S49°49'39"W	82.50'
350 TO 358	N40°09'25"W	195.91'
358 TO 359	SEE ERWC26 DATA	
359 TO 360	N03°16'25"W	169.60'
360 TO 362	N13°07'48"W	62.45'
362 TO 363	N04°25'47"E	161.30'
363 TO 364	N11°08'52"W	60.27'
364 TO 365	N17°47'45"E	55.82'
365 TO 366	N40°15'46"E	107.57'
366 TO 367	N21°44'32"E	170.60'
367 TO 15	N19°08'43"E	95.90'
15 TO 203	N18°48'21"E	19.15'
203 TO 368	N09°20'47"E	182.88'
368 TO 369	S79°07'43"E	66.02'
369 TO 370	S09°20'47"W	83.91'
370 TO 371	S19°05'19"W	122.21'
371 TO 372	S21°44'32"W	75.01'
372 TO 373	S68°36'37"E	12.45'
373 TO 374	S18°34'55"W	281.24'
374 TO 375	SEE ERWC27 DATA	
375 TO 376	S03°16'25"E	54.66'
376 TO 377	N86°43'35"E	16.50'
377 TO 378	S03°16'25"E	170.68'
378 TO 379	SEE ERWC28 DATA	
379 TO 380	S49°50'35"W	8.25'
380 TO 351	S40°09'25"E	195.89'

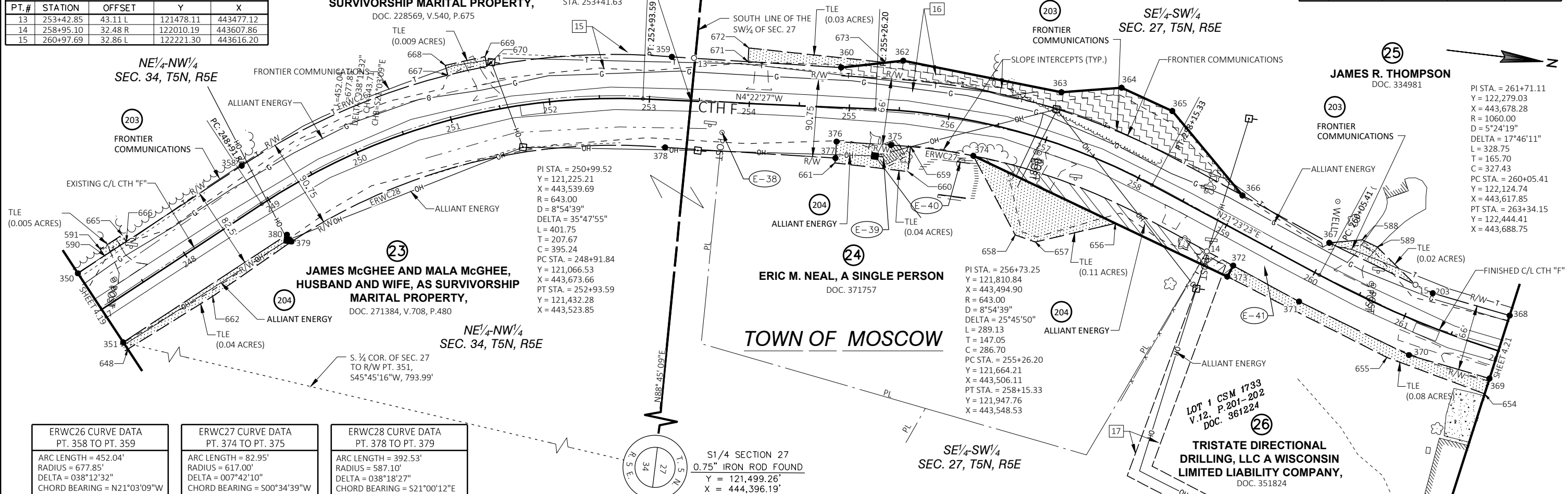
PT.#	STATION	OFFSET	Y	X
588	260+27.43	50.98 L	122162.94	443577.82
589	260+44.87	51.31 L	122178.67	443583.11
590	247+32.00	43.23 L	120916.51	443743.74
591	247+32.00	51.00 L	120911.49	443737.81
648	247+00.00	47.50 R	120950.58	443833.71
654	262+00.00	45.00 R	122302.93	443716.03
655	261+00.00	45.00 R	122201.63	443691.57
656	258+00.00	50.84 R	121916.01	443590.88
657	257+25.00	100.00 R	121838.54	443618.89
658	257+00.00	100.00 R	121817.91	443614.42
659	255+65.00	34.84 R	121703.52	443539.16
660	255+65.00	60.00 R	121703.93	443564.31
661	254+88.00	55.50 R	121630.36	443564.36
662	248+00.00	50.00 R	121028.60	443771.11
665	247+58.00	51.00 L	120931.36	443721.03
666	247+58.00	43.22 L	120936.38	443726.98
667	251+07.50	44.00 L	121235.61	443523.42
668	251+07.50	54.00 L	121232.04	443514.08
669	251+45.00	54.00 L	121270.41	443500.66
670	251+45.00	44.04 L	121273.42	443510.16
671	253+97.00	41.81 L	121532.20	443474.28
672	253+97.00	55.50 L	121531.15	443460.63
673	255+34.00	46.92 L	121668.99	443458.75

PT.#	STATION	OFFSET	Y	X
203	261+17.42	31.88 L	122239.43	443622.37
350	247+00.00	43.24 L	120041.78	443638.04
351	247+00.00	39.26 R	120945.27	443827.42
358	248+95.66	43.20 L	121041.78	443638.04
359	253+20.56	43.28 L	121455.87	443478.65
360	254+90.12	40.02 L	121625.19	443468.96
362	255+50.00	50.00 L	121686.01	443454.78
363	257+00.00	50.00 L	121846.82	443467.23
364	257+50.00	75.00 L	121905.95	443455.58
365	258+00.00	75.00 L	121959.10	443472.64
366	259+00.00	33.61 L	122038.86	443548.12
367	259+98.89	33.00 L	122130.71	443584.75
368	262+00.00	37.13 L	122318.42	443635.37
369	262+00.00	28.89 R	122305.96	443700.21
370	261+18.67	34.35 R	122223.16	443686.58
371	260+00.01	33.01 R	122107.68	443646.62
372	259+25.00	32.55 R	122038.00	443618.83
373	259+25.00	45.00 R	122033.46	443630.43
374	256+32.00	34.20 R	121766.88	443540.81
375	255+44.34	35.03 R	121684.00	443539.97
376	254+88.70	34.22 R	121629.43	443543.09
377	254+88.38	50.71 R	121630.37	443559.56
378	253+17.73	47.44 R	121459.97	443569.31
379	248+96.21	47.55 R	121100.30	443707.40
380	248+96.15	39.30 R	121094.98	443701.09

NOTE: OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO IOWA COUNTY.

PT.#	STATION	OFFSET	Y	X
13	253+42.85	43.11 L	121478.11	443477.12
14	258+95.10	32.48 R	122010.19	443607.86
15	260+97.69	32.86 L	122221.30	443616.20

CHARLES L. HARTLEY, JR. AND TRACEY L. HARTLEY, HUSBAND AND WIFE, AS SURVIVORSHIP MARITAL PROPERTY,
DOC. 228569, V.540, P.675



ERWC26 CURVE DATA PT. 358 TO PT. 359	ERWC27 CURVE DATA PT. 374 TO PT. 375	ERWC28 CURVE DATA PT. 378 TO PT. 379
ARC LENGTH = 452.04' RADIUS = 677.85' DELTA = 038°12'32" CHORD BEARING = N21°03'09"W CHORD LENGTH = 443.71'	ARC LENGTH = 82.95' RADIUS = 617.00' DELTA = 007°42'10" CHORD BEARING = S00°34'39"W CHORD LENGTH = 82.89'	ARC LENGTH = 392.53' RADIUS = 587.10' DELTA = 038°18'27" CHORD BEARING = S21°00'12"E CHORD LENGTH = 385.26'

EASEMENT NUMBER	OWNER	RECORDING INFORMATION	LOCATED IN R/W PARCEL #	REMARKS
15	FRONTIER COMMUNICATIONS (FORMERLY GTE MTO INC)	DOC. 167087, V.399, P.209	22	1 ROD EASEMENT
16	FRONTIER COMMUNICATIONS (FORMERLY GTE NORTH INC)	DOC. 217724, V.513, P.940	25	1 ROD EASEMENT
17	ALLIANT ENERGY (FORMERLY WP&L)	DOC. 350291	26	20' WIDE EASEMENT

ENCROACHMENT	PROPERTY OWNER	LOCATION (STATION/OFFSET)	ENCROACHMENT TYPE
E-38	JOHNATHON FILLION & SERAH JACKSON	STA. 253+74, 31' RT.	POST
E-39	JOHNATHON FILLION & SERAH JACKSON	STA. 255+35 - STA. 255+55, 28.5' RT. - 35' RT.	CONCRETE PAD
E-40	JOHNATHON FILLION & SERAH JACKSON	STA. 255+94 - STA. 256+38, 34.5' RT. - 40' RT.	CELLAR
E-41	TRI STATE DIRECTIONAL DRILLING LLC	STA. 259+26 - STA. 259+91, 28' RT. - 33' RT.	PARKING LOT

REVISION DATE 8-19-21	DATE: MAY 14, 2021	SCALE: FEET 0 50 100	HWY: CTH F	R/W PROJECT NUMBER: 5034-00-03	PLAT SHEET 4.20
	GRID FACTOR N/A		COUNTY: IOWA	CONSTRUCTION PROJECT NUMBER: 5034-00-72	PS&E SHEET E

SCHEDULE OF LANDS & INTERESTS REQUIRED

PARCEL NUMBER	OWNER (S)	INTEREST REQUIRED	HE ACRES REQUIRED NEW	TLE ACRES OR S.F. REQ.
25	JAMES R. THOMPSON	HE	0.40	---
26	TRISTATE DIRECTIONAL DRILLING, LLC A WISCONSIN LIMITED LIABILITY COMPANY	HE, TLE	0.12	0.05
203	FRONTIER COMMUNICATIONS	RELEASE OF RIGHTS		
204	ALLIANT ENERGY	RELEASE OF RIGHTS		

COORDINATE TABLE - FOUND SURVEY MONUMENTS

PT.#	STATION	OFFSET	Y	X
16	262+47.03	37.29 L	122363.18	443642.79
17	409'J'+55.50	33.51 R	122543.18	444046.08

COORDINATE TABLE - TLE POINTS

PT.#	STATION	OFFSET	Y	X
654	262+00.00	45.00 R	122302.93	443716.03

EASEMENT TABLE

EASEMENT NUMBER	OWNER	RECORDING INFORMATION	LOCATED IN R/W PARCEL #	REMARKS
16	FRONTIER COMMUNICATIONS (FORMERLY GTE NORTH INC)	DOC. 217724, V.513, P.940	25	1 ROD EASEMENT
17	ALLIANT ENERGY (FORMERLY WP&L)	DOC. 350291	26	20' WIDE EASEMENT

ENCROACHMENT TABLE

ENCROACHMENT	PROPERTY OWNER	LOCATION (STATION/OFFSET)	ENCROACHMENT TYPE
E-42	JAMES THOMPSON	STA. 262+46 - STA.265+05, 38' LT. - 45' LT. STA. 404'J'+87 - STA. 405'J'+37, 28' RT. - 33' RT.	FENCE

NOTE: OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO IOWA COUNTY.

RIGHT OF WAY LINE TABLE

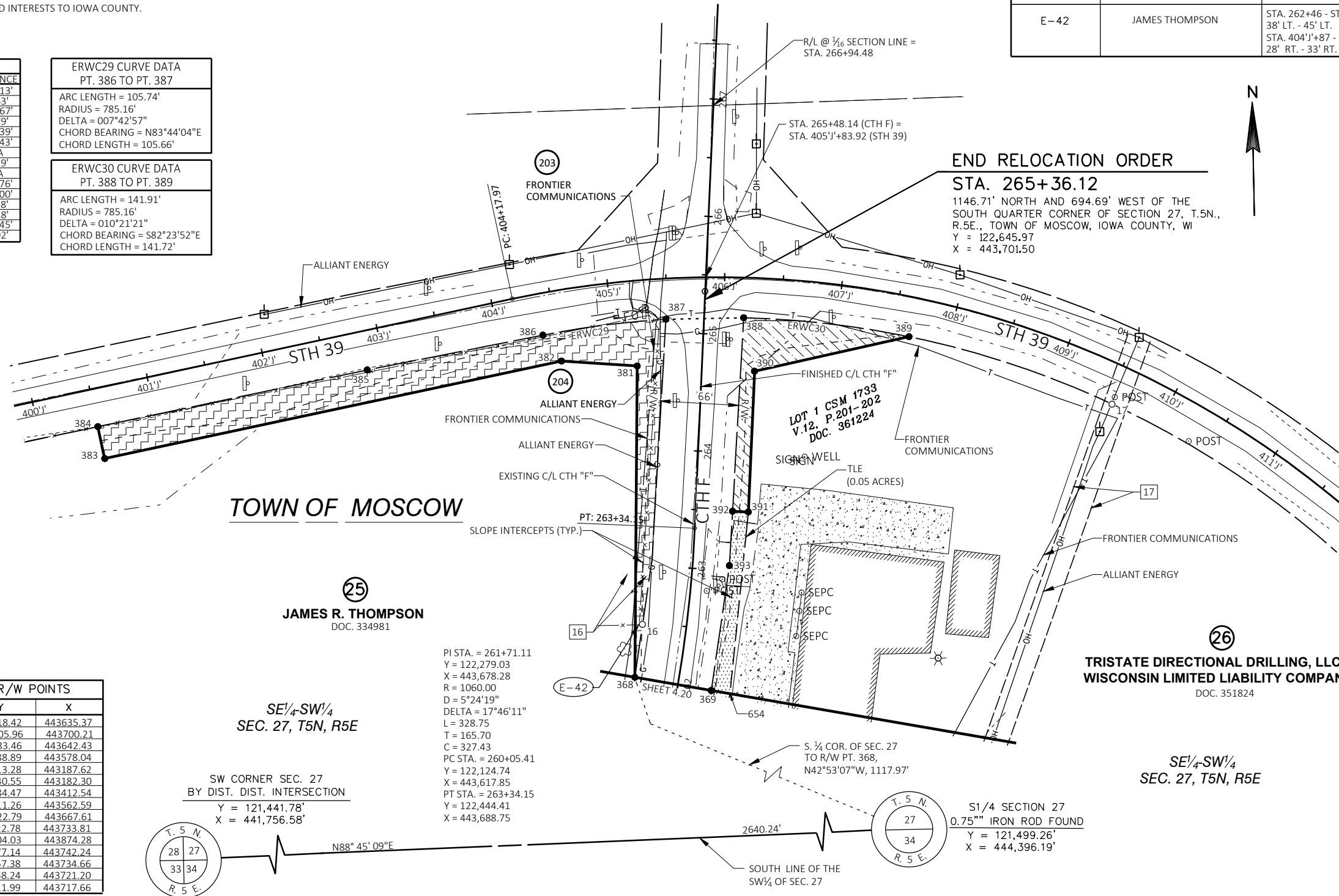
POINT TO POINT	BEARING	DISTANCE
368 TO 381	N01°31'33"E	265.13'
381 TO 382	N85°10'37"W	64.63'
382 TO 383	S79°02'21"W	397.67'
383 TO 384	N11°02'37"W	27.79'
384 TO 385	N79°12'06"E	234.39'
385 TO 386	N79°52'36"E	152.43'
386 TO 387	SEE ERWC29 DATA	
387 TO 388	S89°59'30"E	66.19'
388 TO 389	SEE ERWC30 DATA	
389 TO 390	S78°29'19"W	134.76'
390 TO 391	S03°37'12"W	120.00'
391 TO 392	N86°21'46"W	13.48'
392 TO 393	S04°22'45"W	46.38'
393 TO 369	S09°20'47"W	107.45'
369 TO 368	N79°07'43"W	66.02'

**ERWC29 CURVE DATA
PT. 386 TO PT. 387**

ARC LENGTH = 105.74'
RADIUS = 785.16'
DELTA = 007°42'57"
CHORD BEARING = N83°44'04"E
CHORD LENGTH = 105.66'

**ERWC30 CURVE DATA
PT. 388 TO PT. 389**

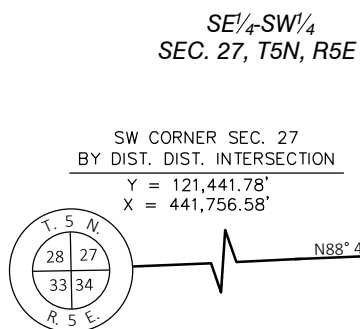
ARC LENGTH = 141.91'
RADIUS = 785.16'
DELTA = 010°21'21"
CHORD BEARING = S82°23'52"E
CHORD LENGTH = 141.72'



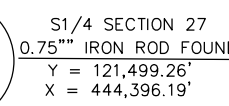
**END RELOCATION ORDER
STA. 265+36.12**
1146.71' NORTH AND 694.69' WEST OF THE SOUTH QUARTER CORNER OF SECTION 27, T.5N., R.5E., TOWN OF MOSCOW, IOWA COUNTY, WI
Y = 122,645.97
X = 443,701.50

COORDINATE TABLE - NEW R/W POINTS

PT.#	STATION	OFFSET	Y	X
368	262+00.00	37.13 L	122318.42	443635.37
369	262+00.00	28.89 R	122305.96	443700.21
381	264+70.00	55.00 L	122583.46	443642.43
382	404'J'+50.00	60.00 R	122588.89	443578.04
383	400'J'+50.00	60.00 R	122513.28	443187.62
384	400'J'+50.00	32.21 R	122540.55	443182.30
385	402'J'+84.39	33.21 R	122584.47	443412.54
386	404'J'+37.64	35.43 R	122611.26	443562.59
387	265+10.85	32.35 L	122622.79	443667.61
388	265+15.02	33.71 R	122622.78	443733.81
389	407'J'+65.11	33.68 R	122604.03	443874.28
390	264+70.00	45.00 R	122577.14	443742.24
391	263+50.00	45.00 R	122457.38	443734.66
392	263+50.00	31.52 R	122458.24	443721.20
393	263+04.50	31.33 R	122411.99	443717.66

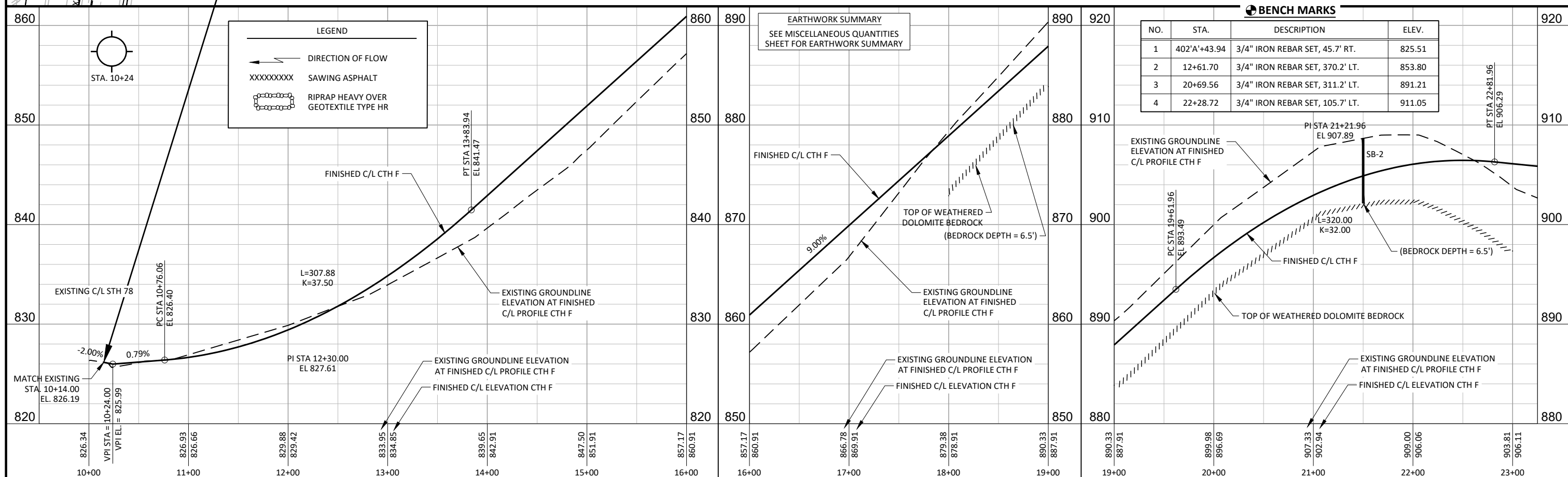
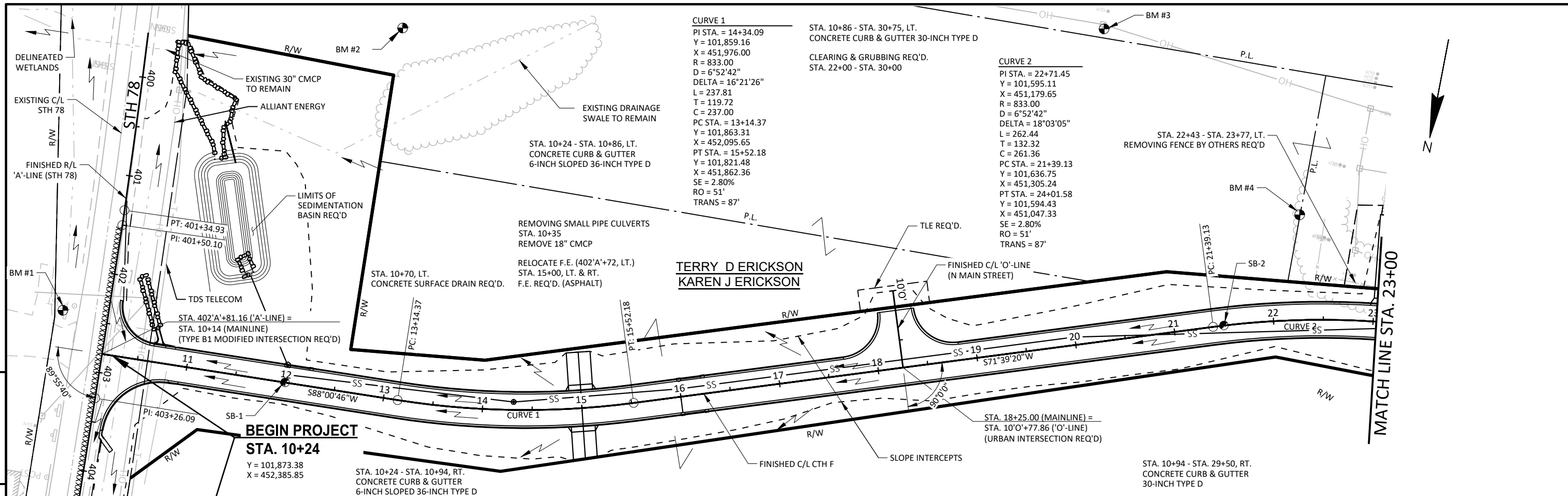


PI STA. = 261+71.11
Y = 122,279.03
X = 443,678.28
R = 1060.00
D = 5°24'19"
DELTA = 17°46'11"
L = 328.75
T = 165.70
C = 327.43
PC STA. = 260+05.41
Y = 122,124.74
X = 443,617.85
PT STA. = 263+34.15
Y = 122,444.41
X = 443,688.75



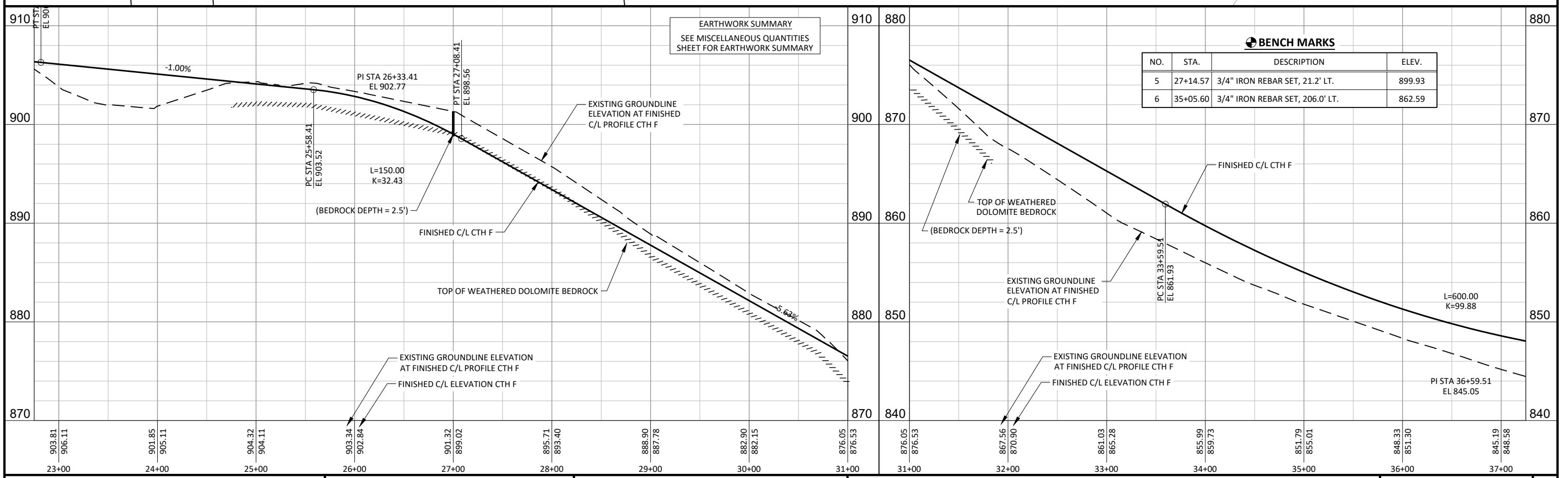
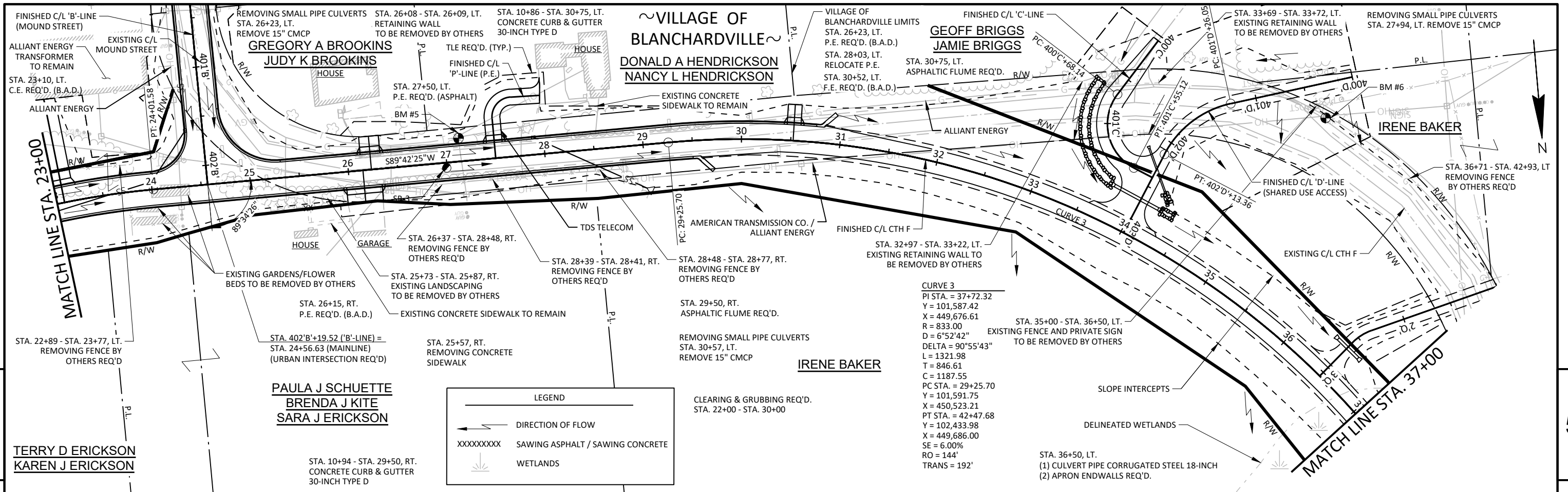
NOTE: EXISTING C/L OF CTH F BASED ON CENTERLINE OF EXISTING PAVEMENT.
EXISTING RIGHT-OF-WAY FOR CTH F BASED ON THE CENTERLINE OF EXISTING PAVEMENT AND WIS. STATUTE 82.31(2).

REVISION DATE 8-19-21	DATE: MAY 14, 2021	SCALE, FEET 0 50 100	HWY: CTH F	R/W PROJECT NUMBER: 5034-00-03	PLAT SHEET 4.21
GRID FACTOR N/A			COUNTY: IOWA	CONSTRUCTION PROJECT NUMBER: 5034-00-72	PS&E SHEET E



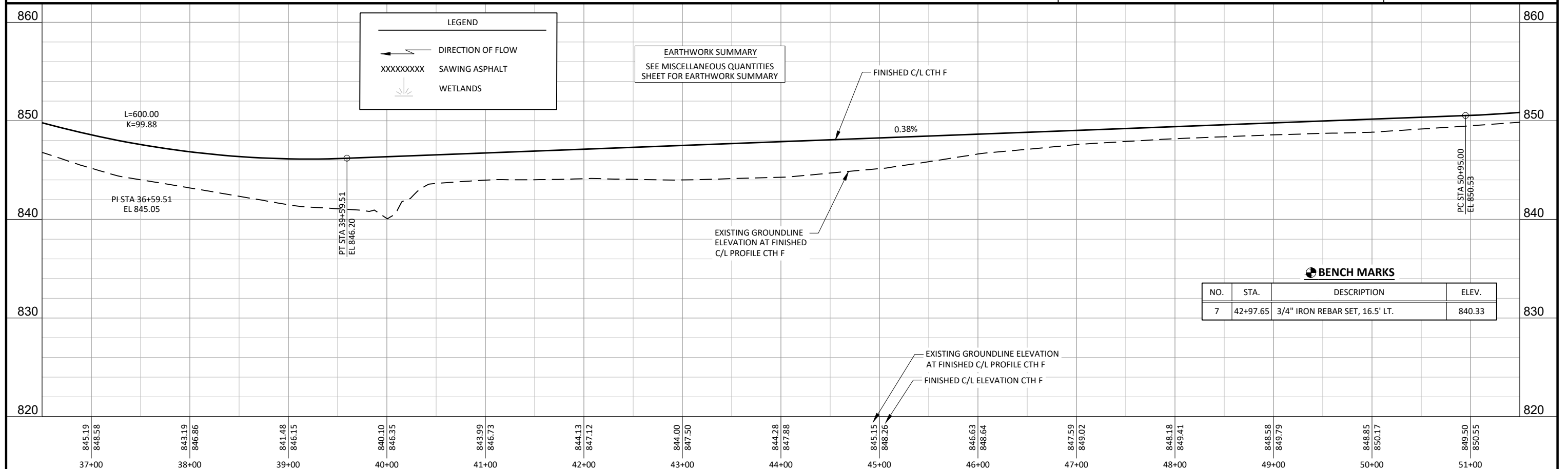
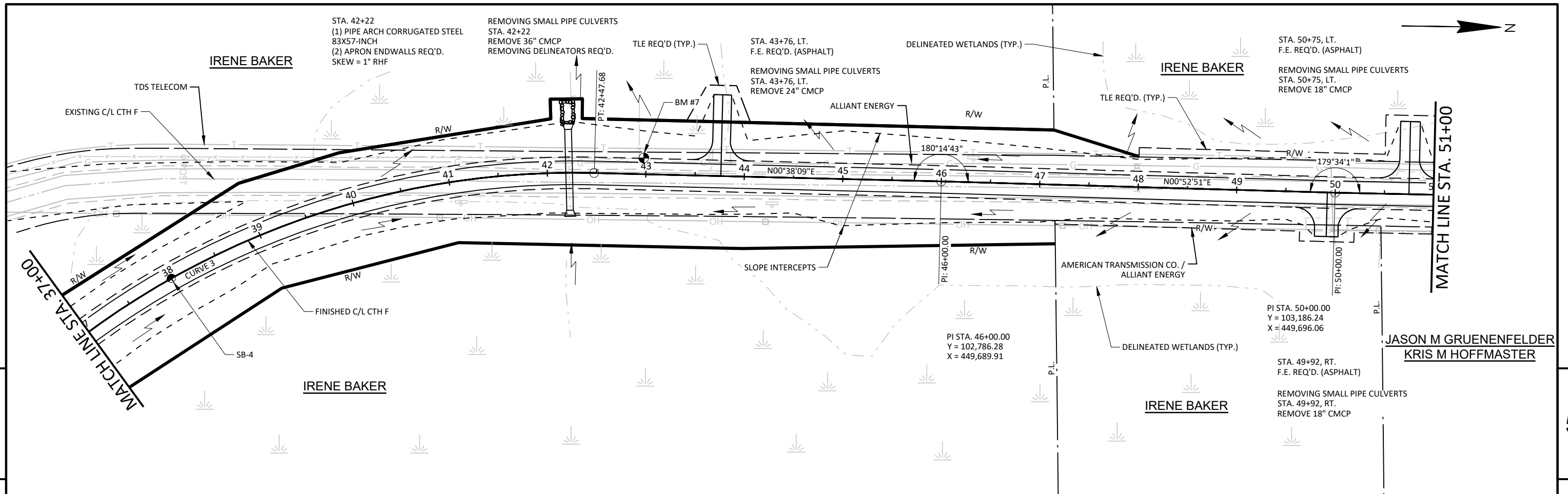
BENCH MARKS			
NO.	STA.	DESCRIPTION	ELEV.
1	402'A+43.94	3/4" IRON REBAR SET, 45.7' RT.	825.51
2	12+61.70	3/4" IRON REBAR SET, 370.2' LT.	853.80
3	20+69.56	3/4" IRON REBAR SET, 311.2' LT.	891.21
4	22+28.72	3/4" IRON REBAR SET, 105.7' LT.	911.05

PROJECT NO: 5034-00-72 HWY: CTH F COUNTY: IOWA PLAN AND PROFILE: MAINLINE SHEET: E

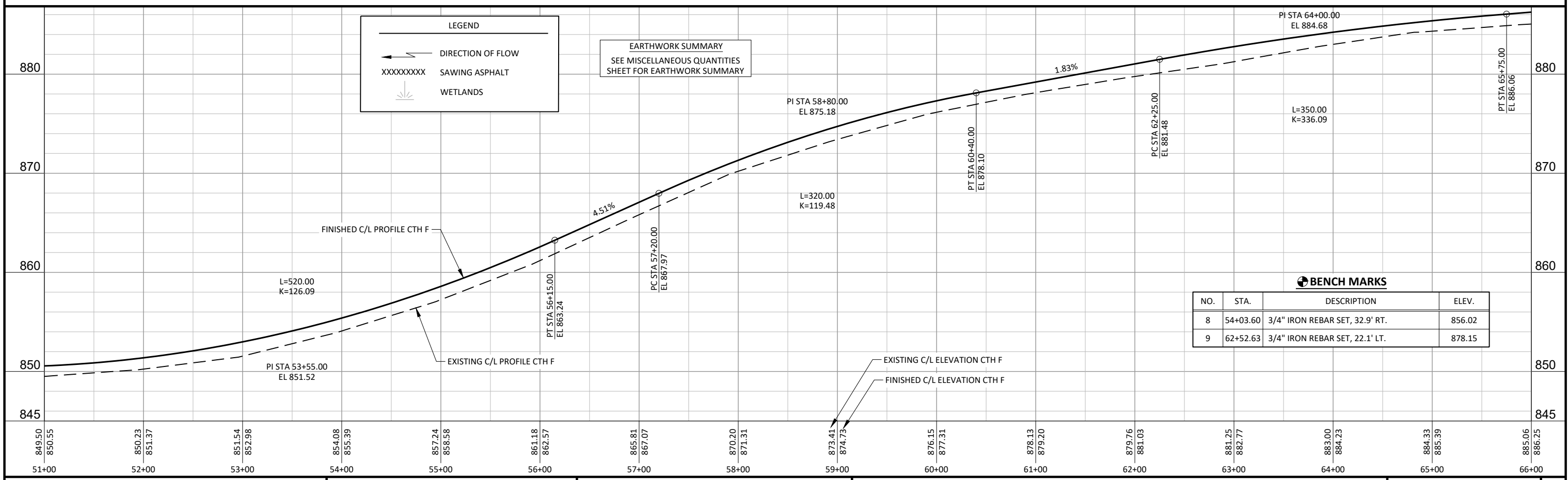
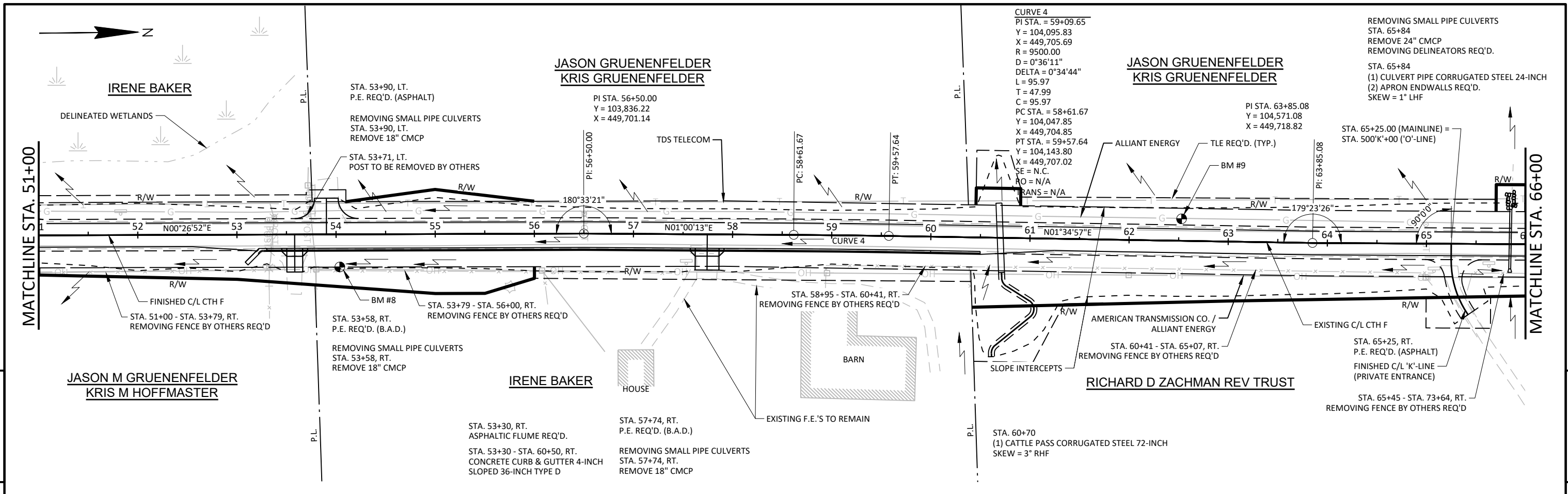


BENCH MARKS			
NO.	STA.	DESCRIPTION	ELEV.
5	27+14.57	3/4" IRON REBAR SET, 21.2' LT.	899.93
6	35+05.60	3/4" IRON REBAR SET, 206.0' LT.	862.59

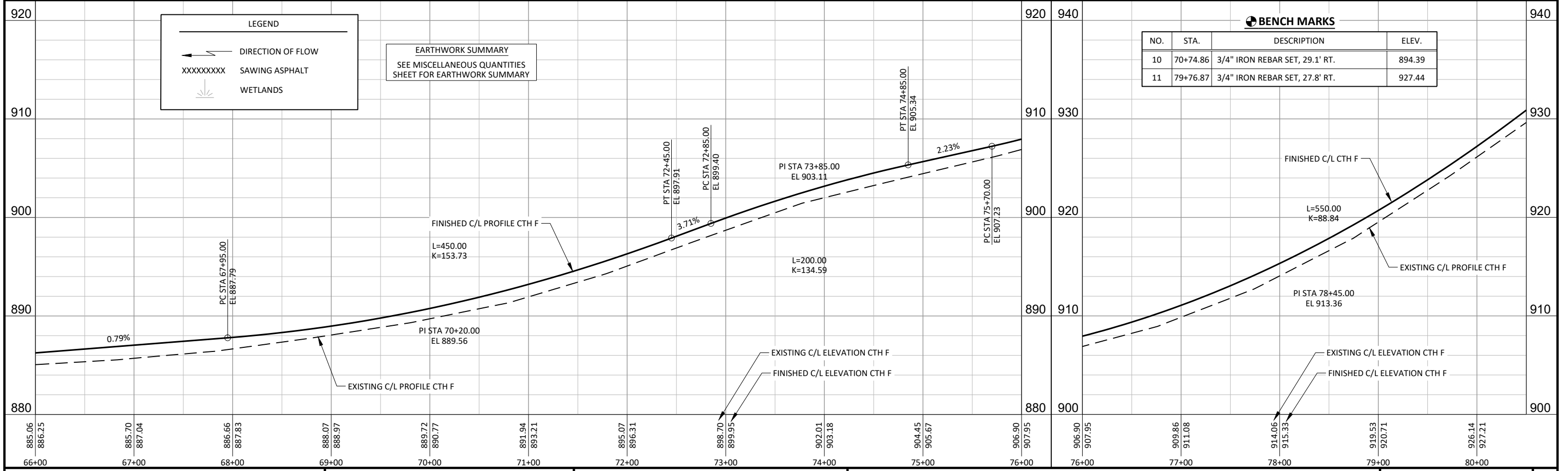
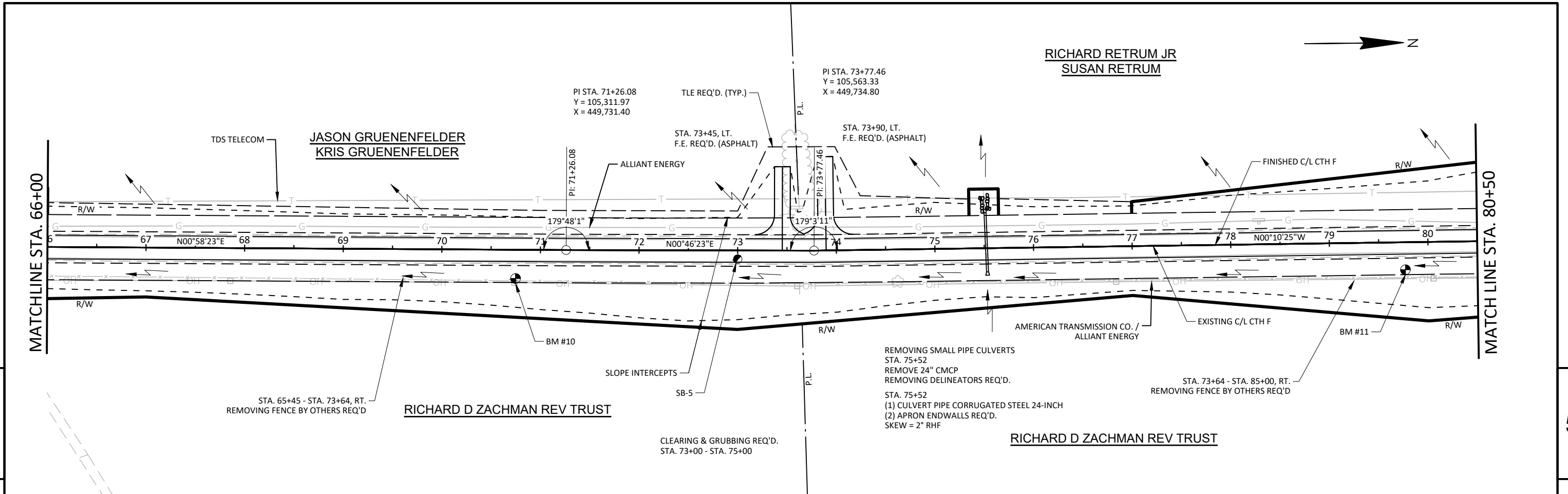
PROJECT NO: 5034-00-72 HWY: CTH F COUNTY: IOWA PLAN AND PROFILE: MAINLINE SHEET: 5



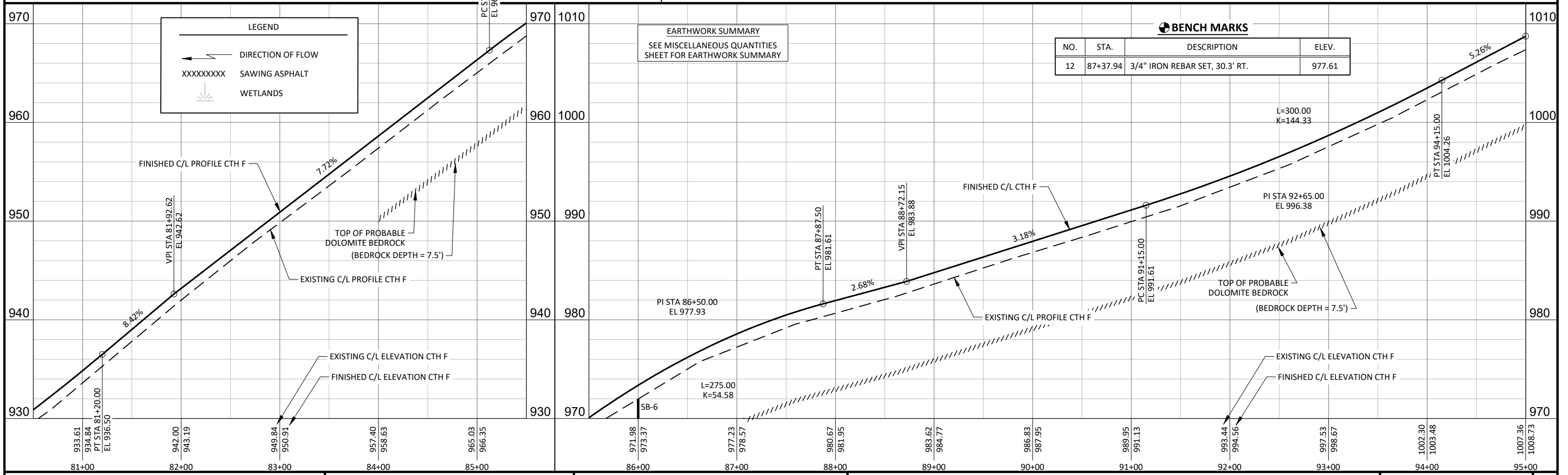
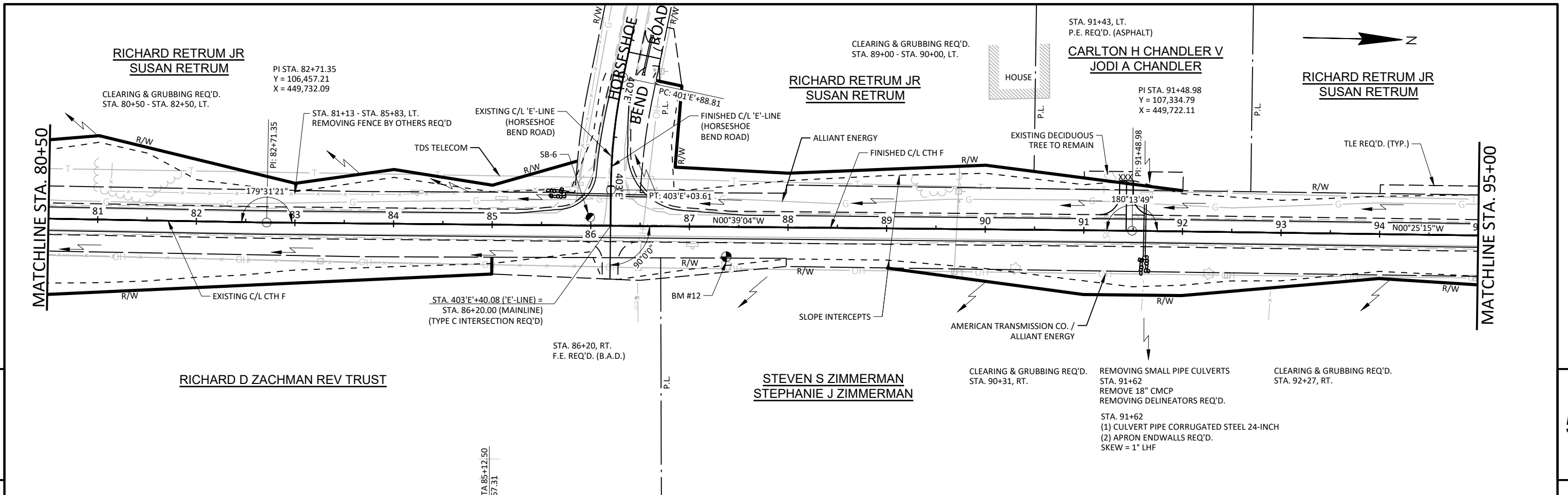
PROJECT NO: 5034-00-72 HWY: CTH F COUNTY: IOWA PLAN AND PROFILE: MAINLINE SHEET: 5



PROJECT NO: 5034-00-72 HWY: CTH F COUNTY: IOWA PLAN AND PROFILE: MAINLINE SHEET: E

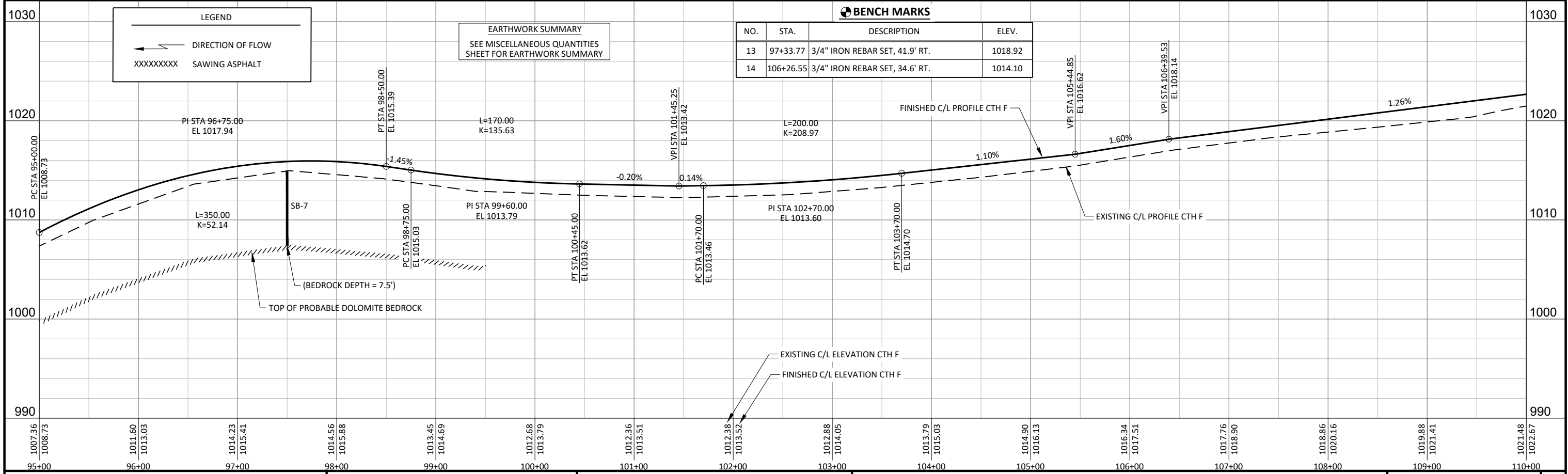
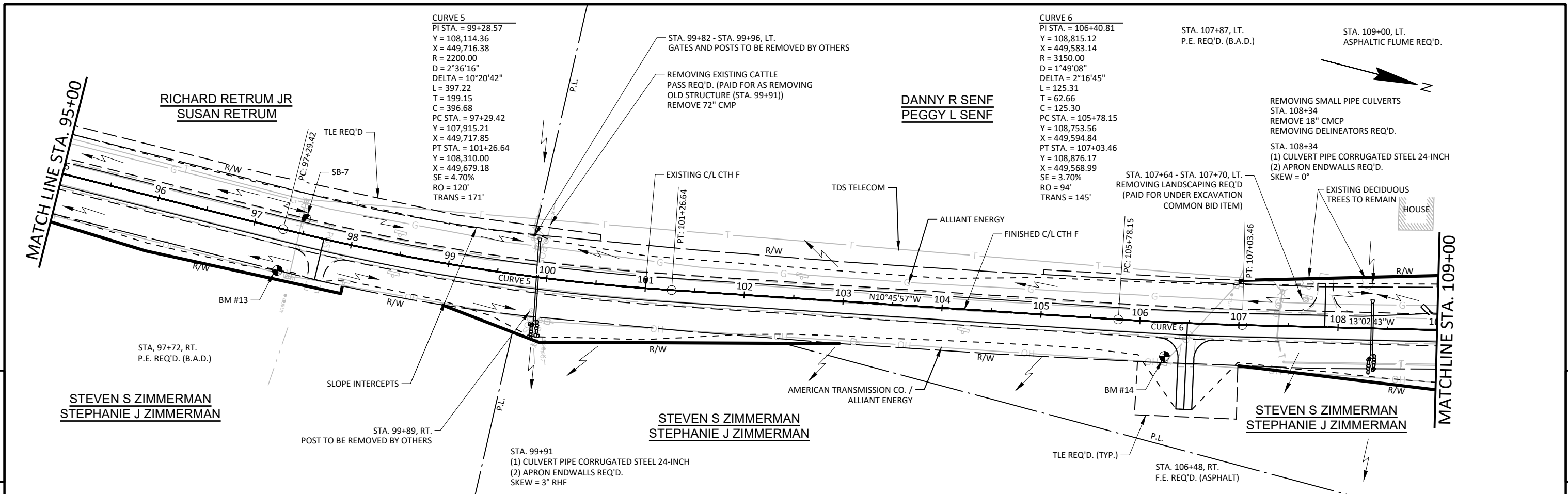


PROJECT NO: 5034-00-72 HWY: CTH F COUNTY: IOWA PLAN AND PROFILE: MAINLINE SHEET: 5

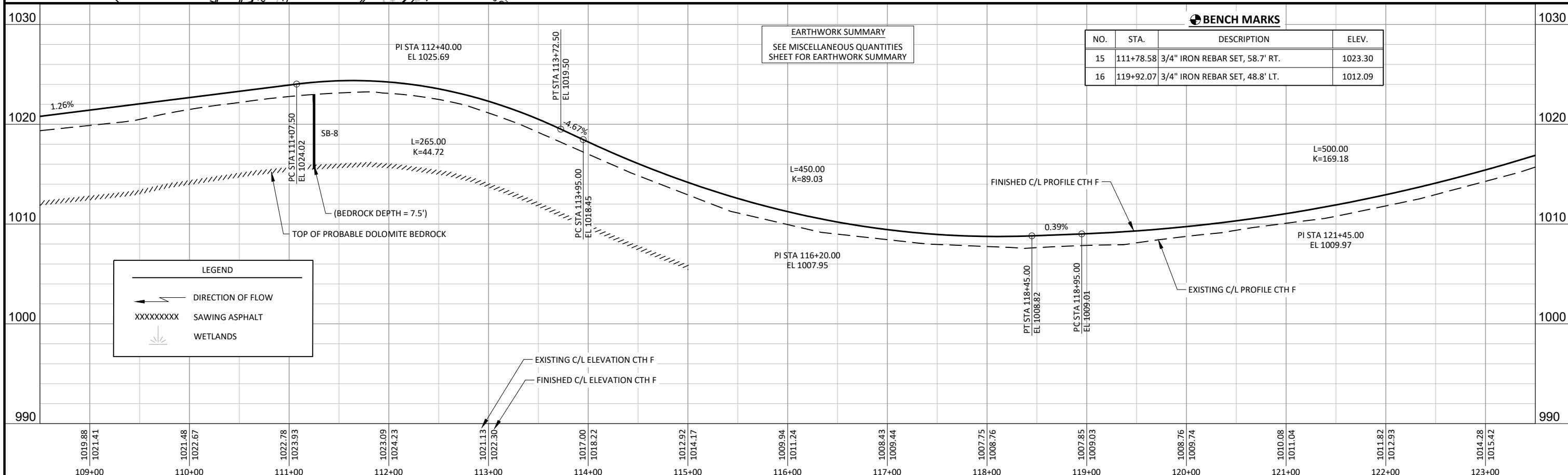
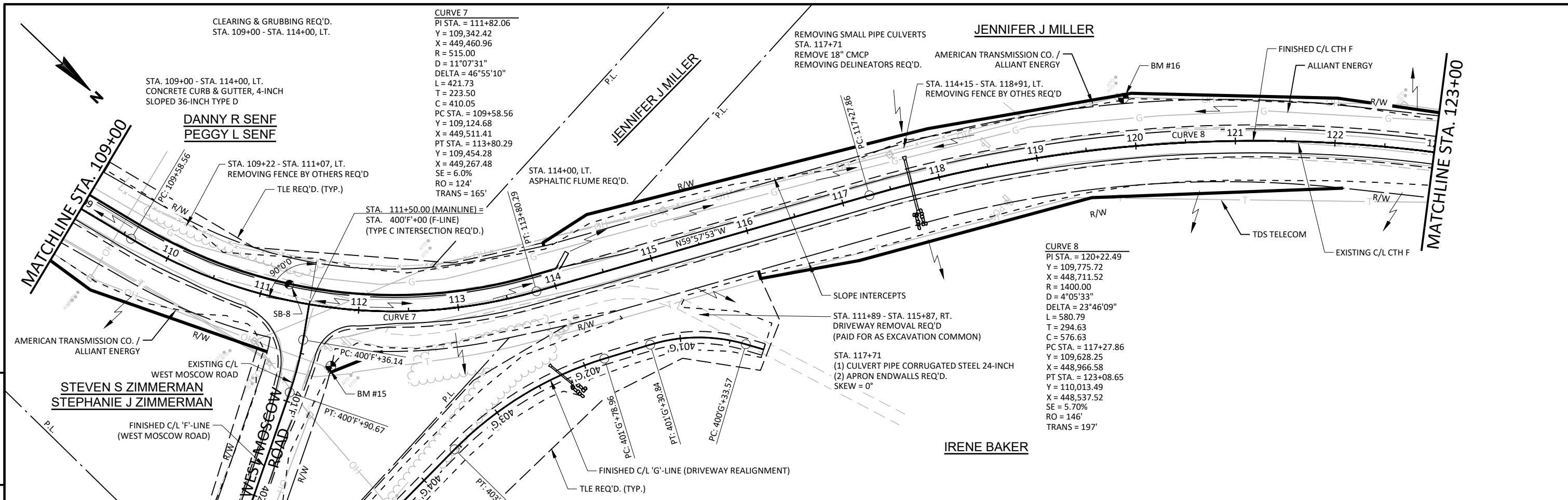


BENCH MARKS			
NO.	STA.	DESCRIPTION	ELEV.
12	87+37.94	3/4" IRON REBAR SET, 30.3' RT.	977.61

PROJECT NO: 5034-00-72 HWY: CTH F COUNTY: IOWA PLAN AND PROFILE: MAINLINE SHEET: E



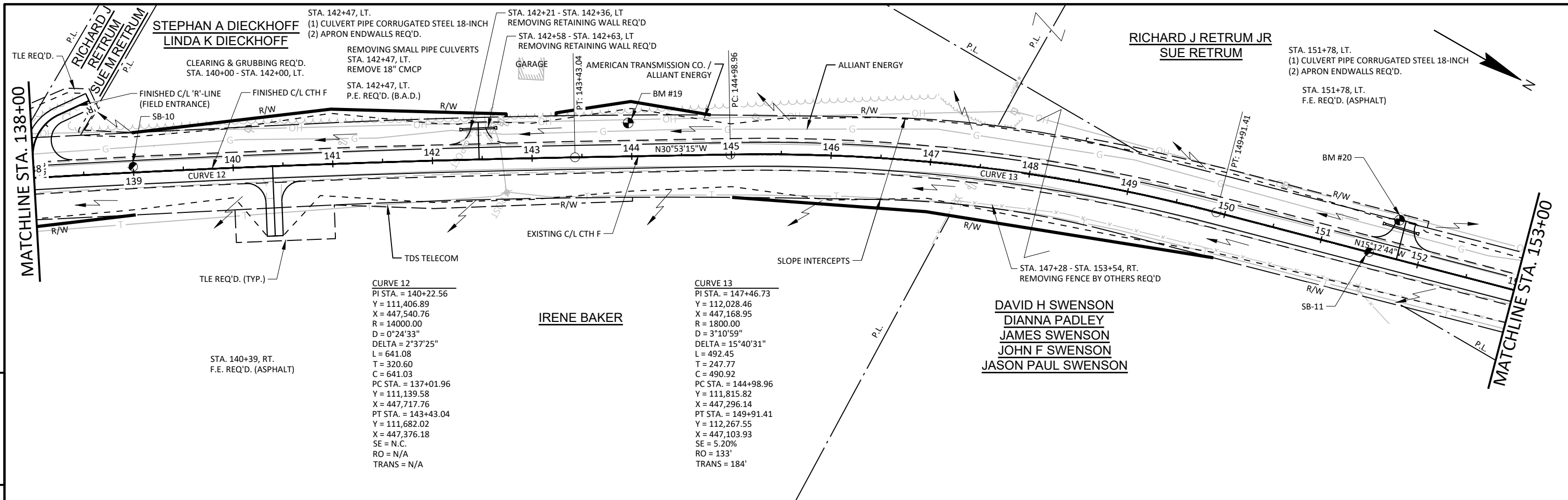
PROJECT NO: 5034-00-72 HWY: CTH F COUNTY: IOWA PLAN AND PROFILE: MAINLINE SHEET: E



BENCH MARKS

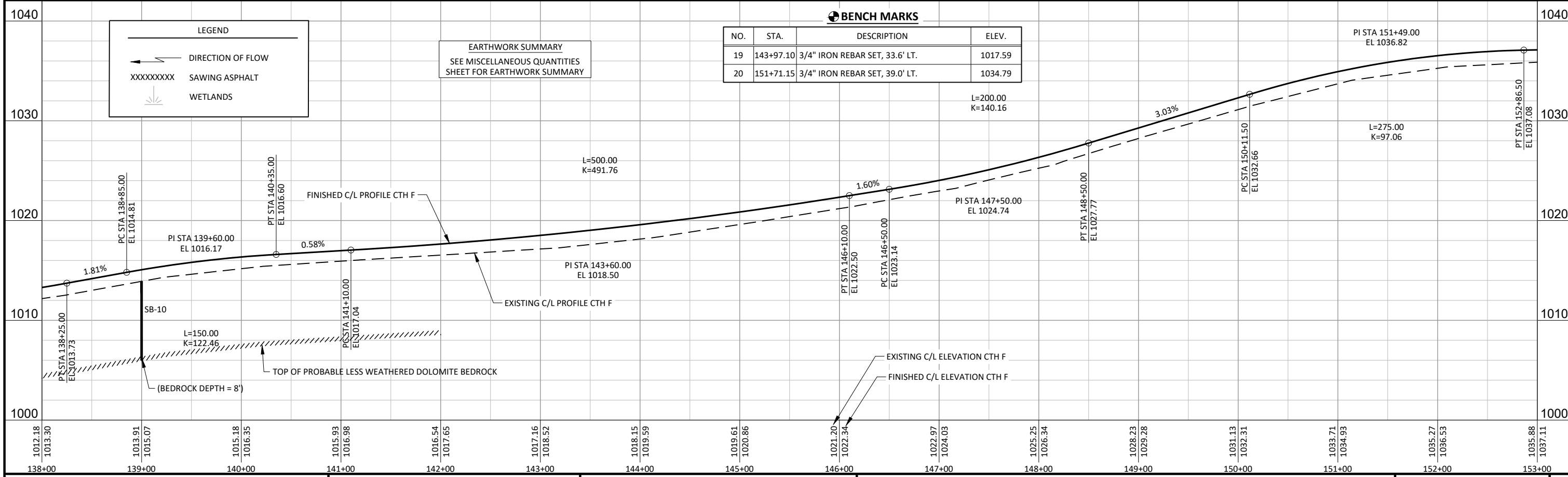
NO.	STA.	DESCRIPTION	ELEV.
15	111+78.58	3/4" IRON REBAR SET, 58.7' RT.	1023.30
16	119+92.07	3/4" IRON REBAR SET, 48.8' LT.	1012.09

PROJECT NO: 5034-00-72 HWY: CTH F COUNTY: IOWA PLAN AND PROFILE: MAINLINE SHEET: 5

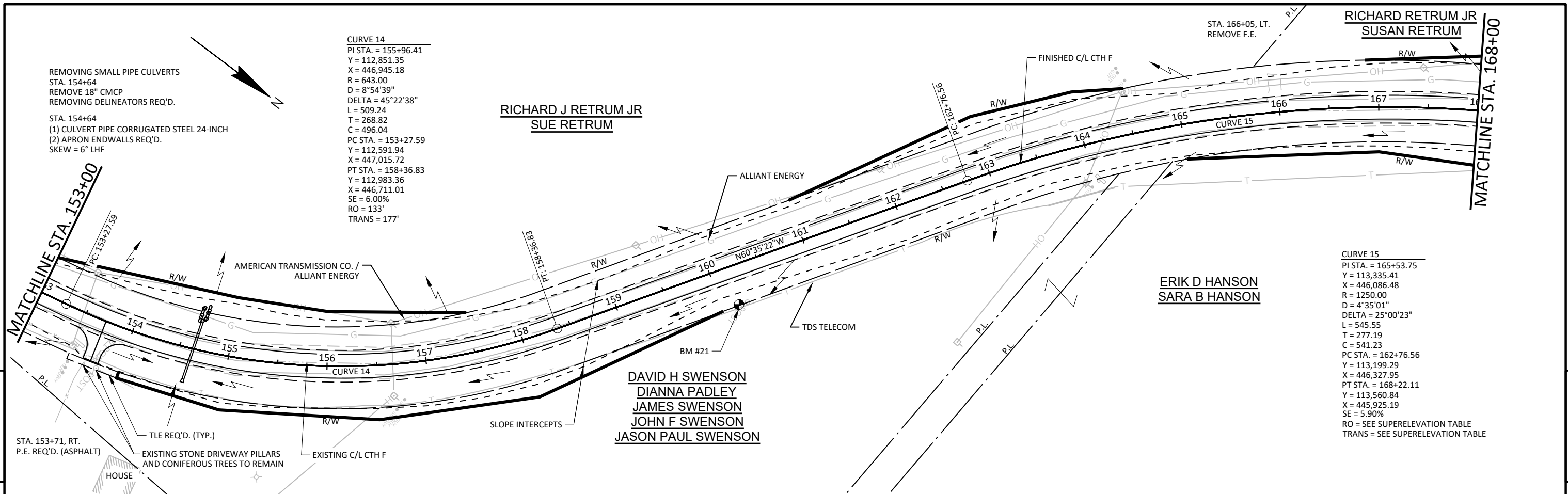


5

5



PROJECT NO: 5034-00-72 HWY: CTH F COUNTY: IOWA PLAN AND PROFILE: MAINLINE SHEET E

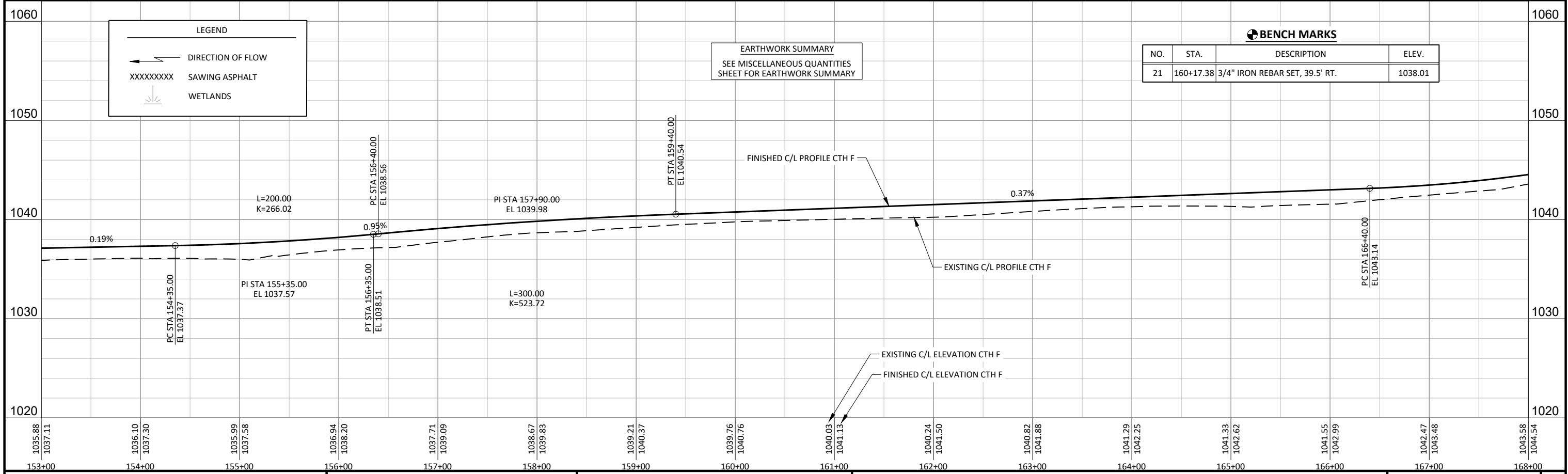


CURVE 14
 PI STA. = 155+96.41
 Y = 112,851.35
 X = 446,945.18
 R = 643.00
 D = 8°54'39"
 DELTA = 45°22'38"
 L = 509.24
 T = 268.82
 C = 496.04
 PC STA. = 153+27.59
 Y = 112,591.94
 X = 447,015.72
 PT STA. = 158+36.83
 Y = 112,983.36
 X = 446,711.01
 SE = 6.00%
 RO = 133'
 TRANS = 177'

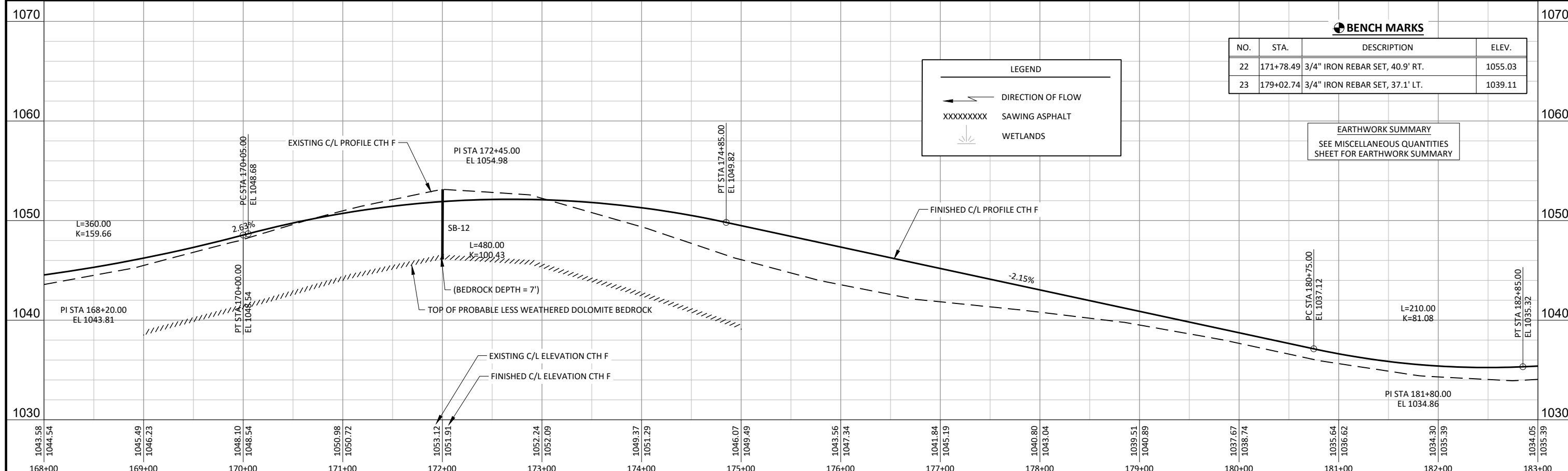
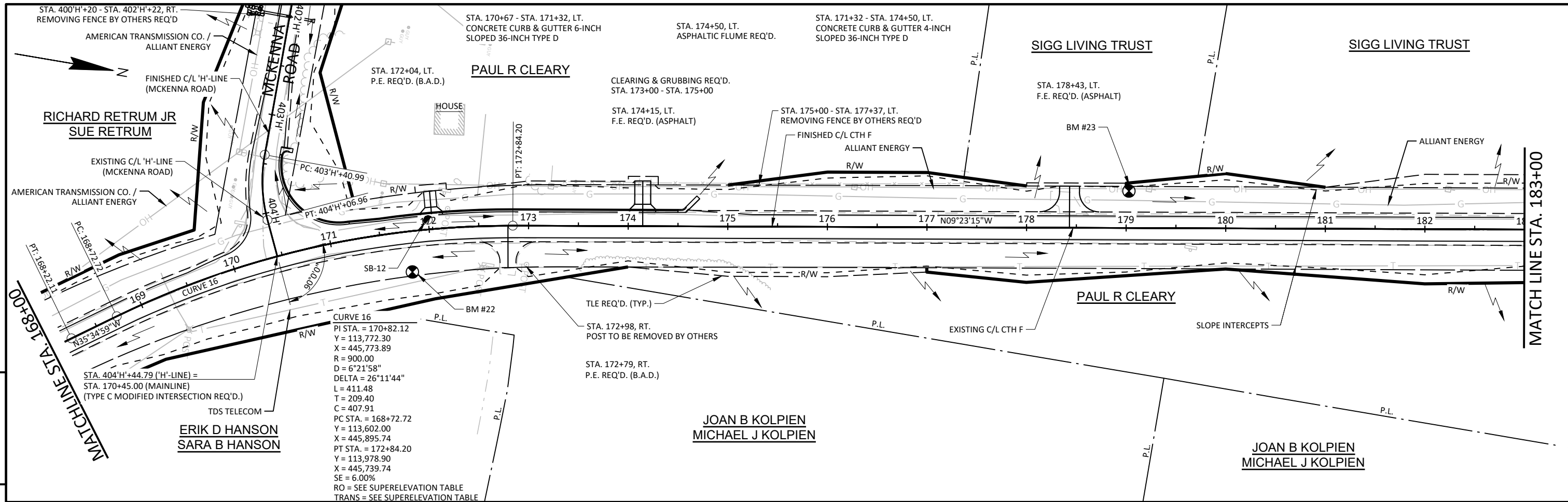
CURVE 15
 PI STA. = 165+53.75
 Y = 113,335.41
 X = 446,086.48
 R = 1250.00
 D = 4°35'01"
 DELTA = 25°00'23"
 L = 545.55
 T = 277.19
 C = 541.23
 PC STA. = 162+76.56
 Y = 113,199.29
 X = 446,327.95
 PT STA. = 168+22.11
 Y = 113,560.84
 X = 445,925.19
 SE = 5.90%
 RO = SEE SUPERELEVATION TABLE
 TRANS = SEE SUPERELEVATION TABLE

5

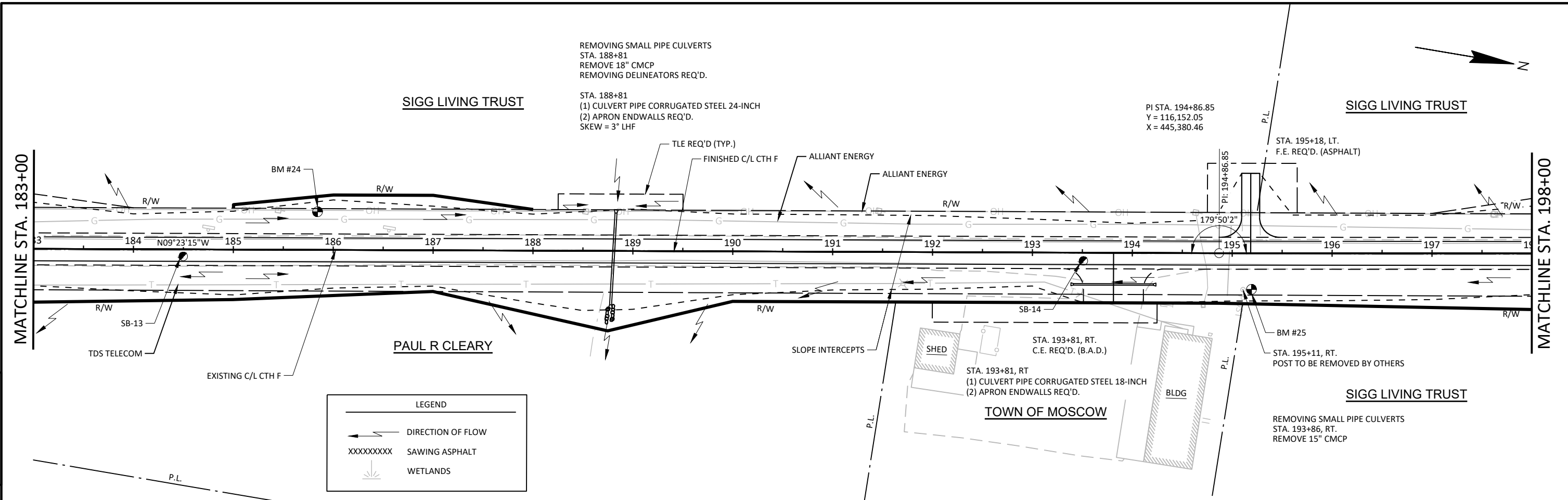
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PROJECT NO: 5034-00-72 HWY: CTH F COUNTY: IOWA PLAN AND PROFILE: MAINLINE SHEET: E

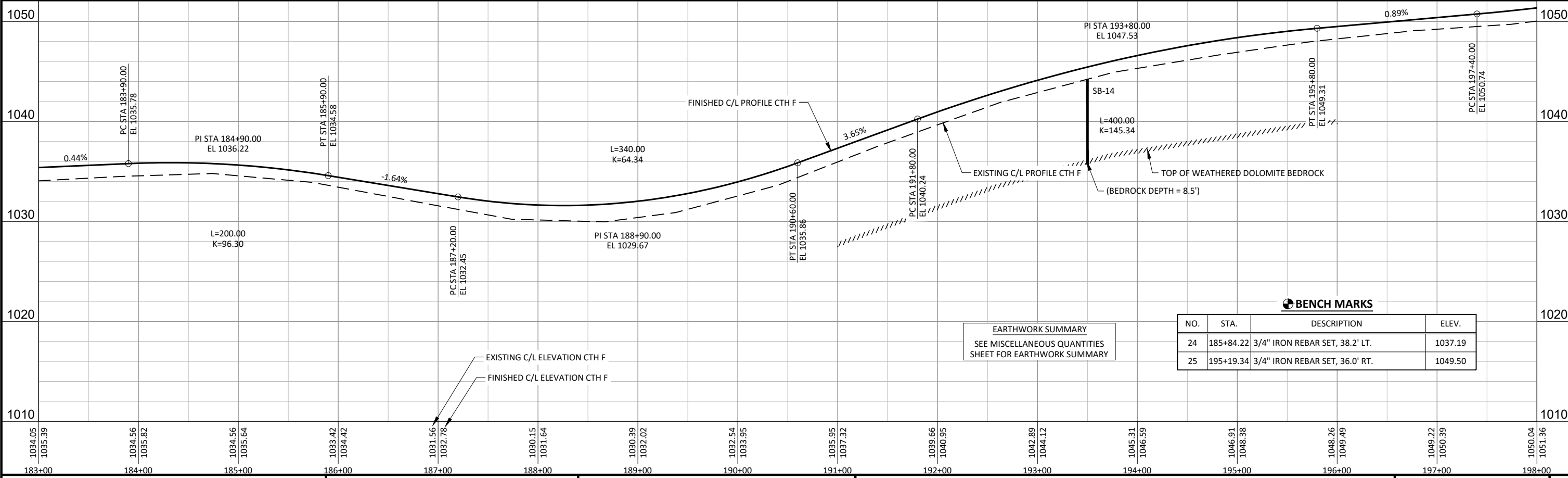


PROJECT NO: 5034-00-72 HWY: CTH F COUNTY: IOWA PLAN AND PROFILE: MAINLINE SHEET: 5



LEGEND

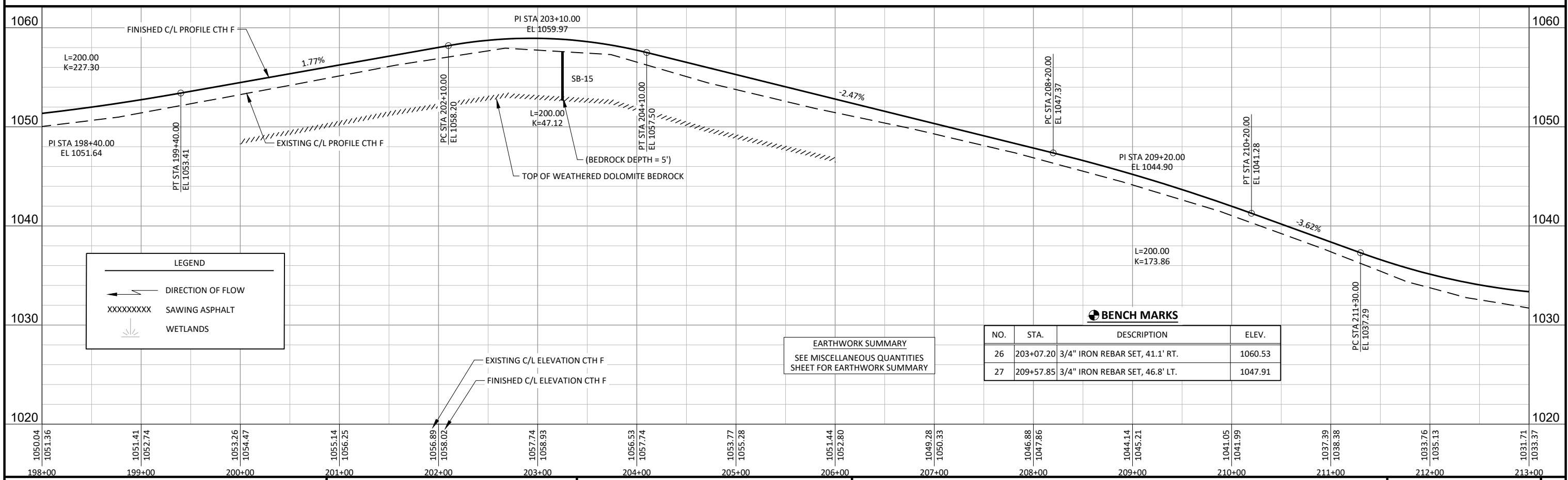
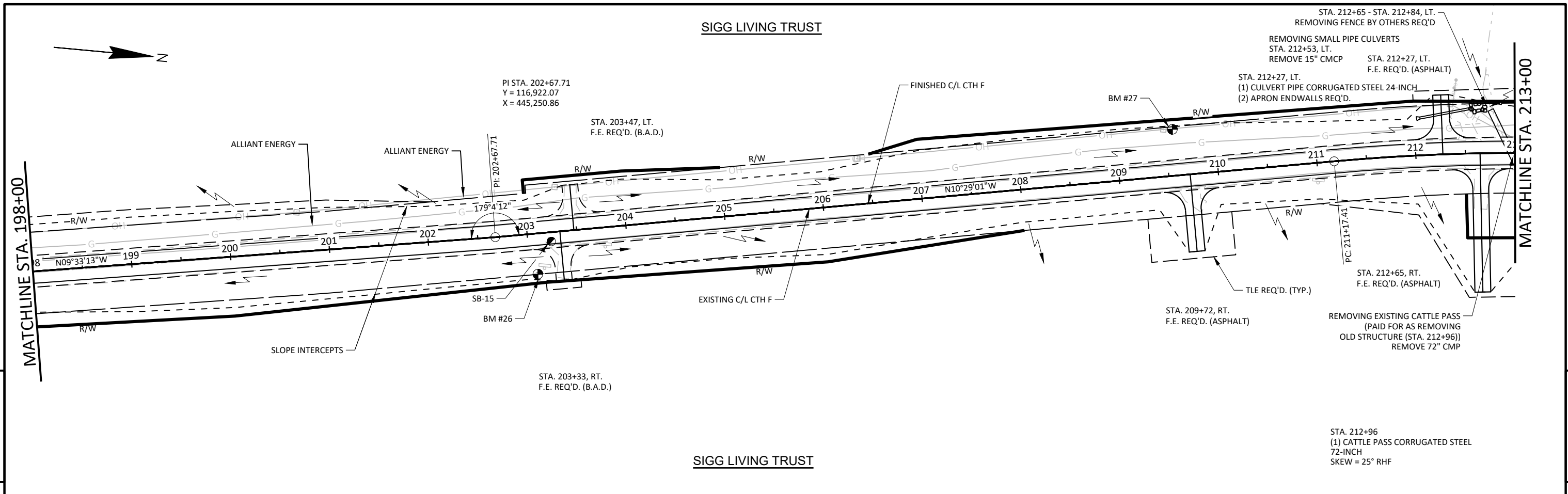
- DIRECTION OF FLOW
- SAWING ASPHALT
- WETLANDS



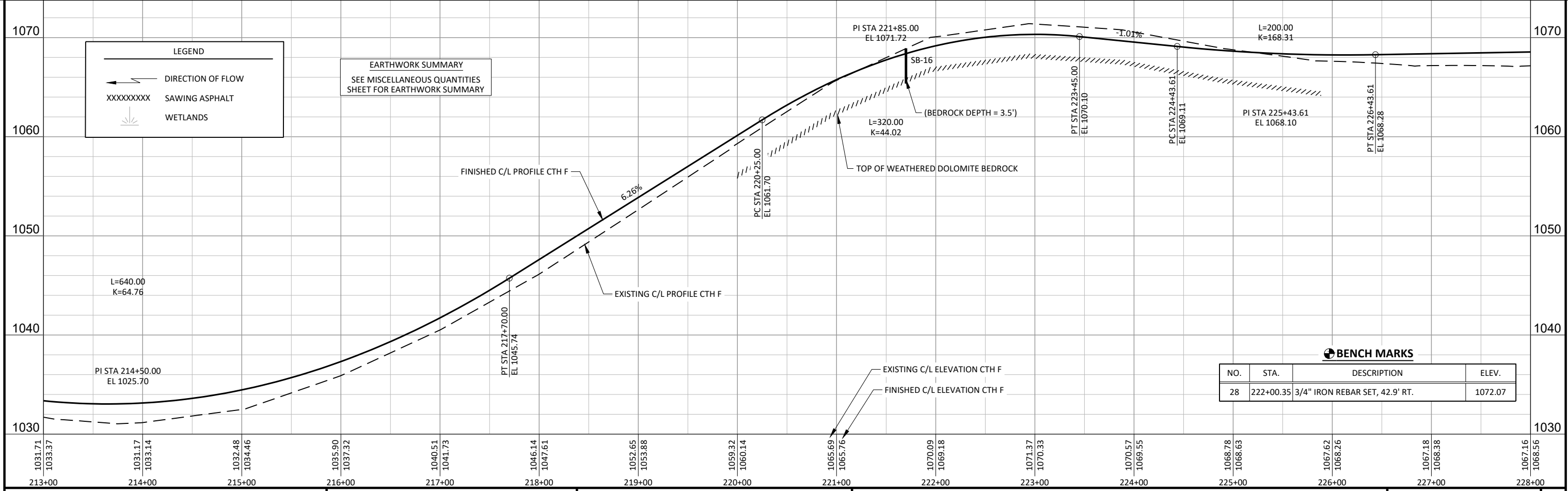
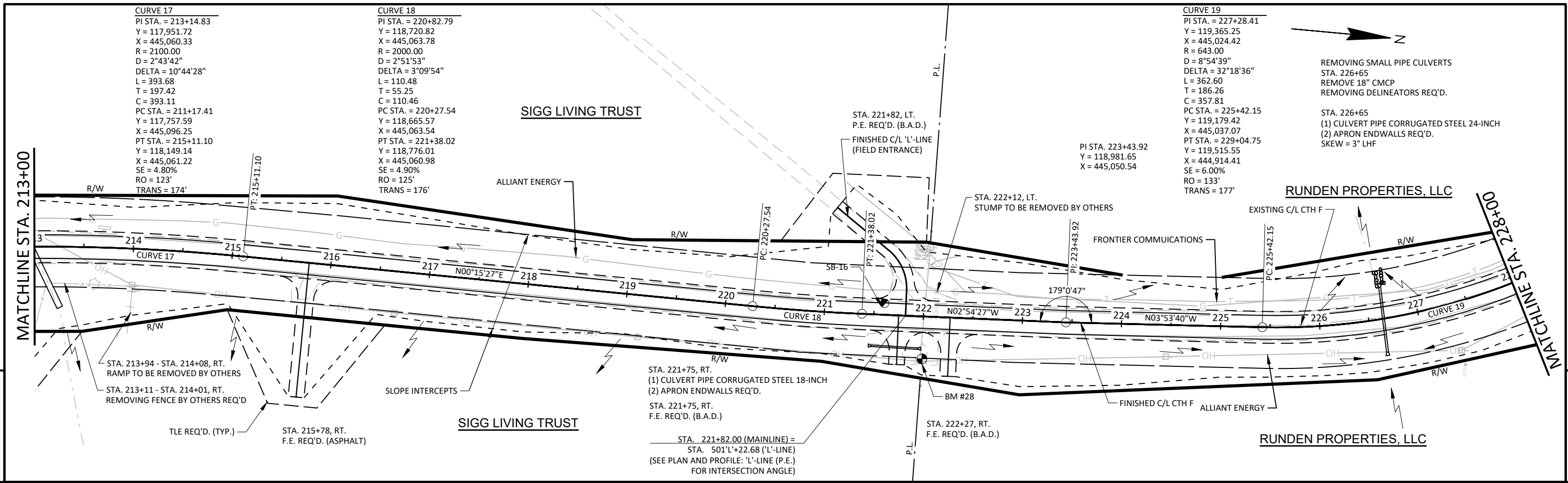
EARTHWORK SUMMARY
SEE MISCELLANEOUS QUANTITIES SHEET FOR EARTHWORK SUMMARY

BENCH MARKS

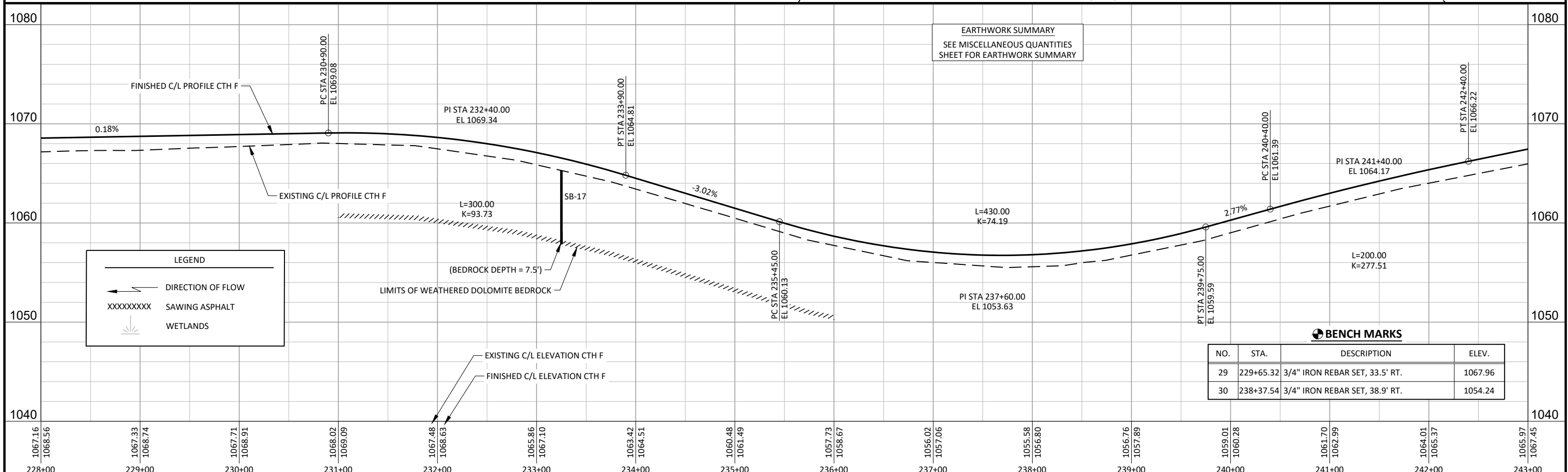
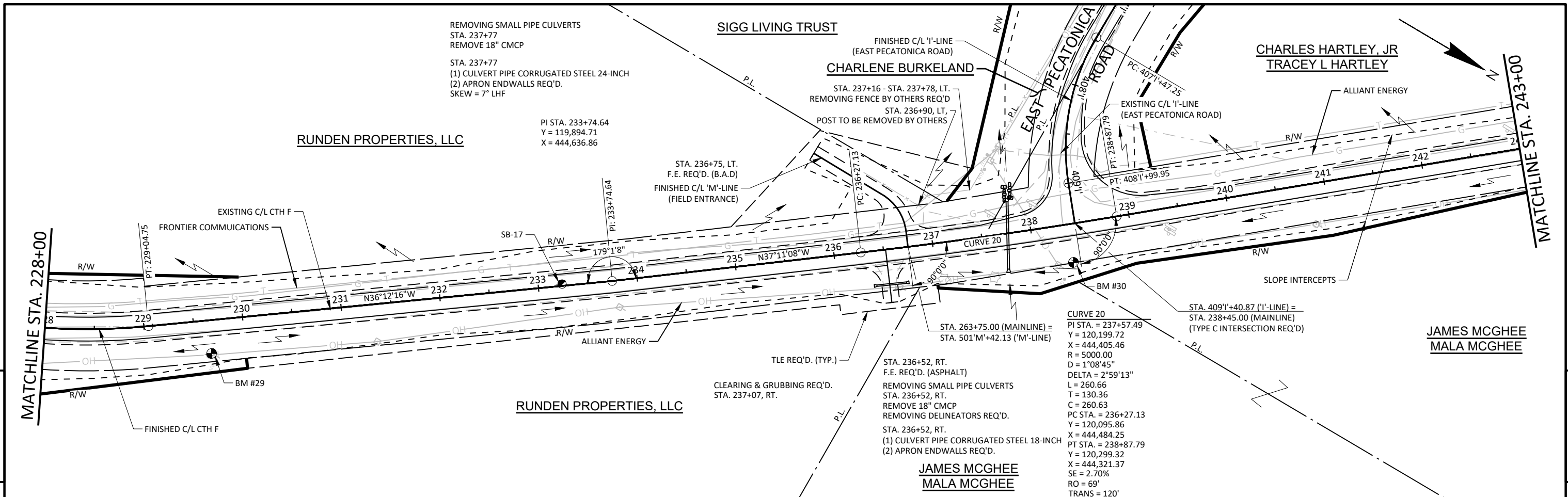
NO.	STA.	DESCRIPTION	ELEV.
24	185+84.22	3/4" IRON REBAR SET, 38.2' LT.	1037.19
25	195+19.34	3/4" IRON REBAR SET, 36.0' RT.	1049.50



PROJECT NO: 5034-00-72 | HWY: CTH F | COUNTY: IOWA | PLAN AND PROFILE: MAINLINE | SHEET | E



PROJECT NO: 5034-00-72 HWY: CTH F COUNTY: IOWA PLAN AND PROFILE: MAINLINE SHEET 5



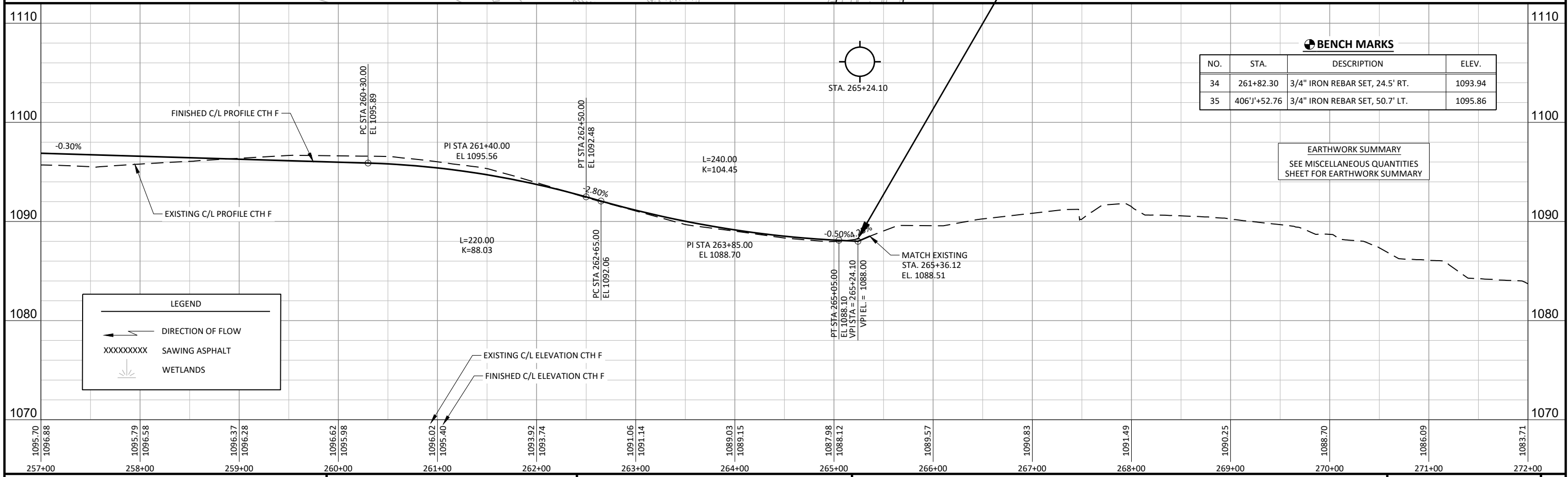
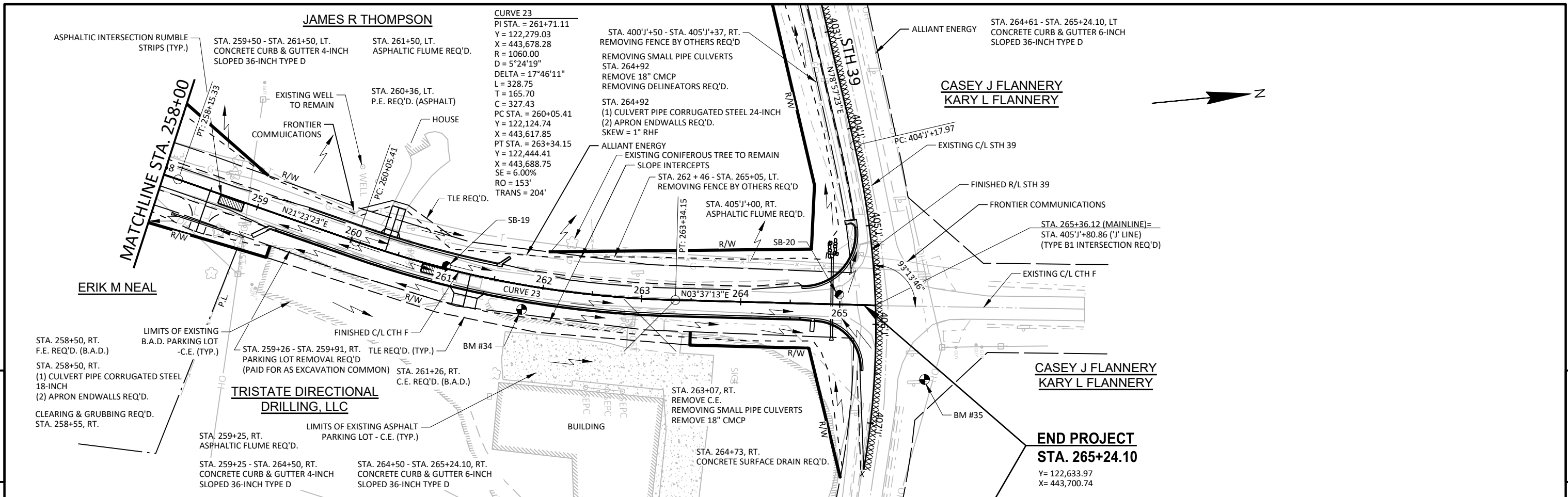
LEGEND

- DIRECTION OF FLOW
- SAWING ASPHALT
- WETLANDS

BENCH MARKS

NO.	STA.	DESCRIPTION	ELEV.
29	229+65.32	3/4" IRON REBAR SET, 33.5' RT.	1067.96
30	238+37.54	3/4" IRON REBAR SET, 38.9' RT.	1054.24

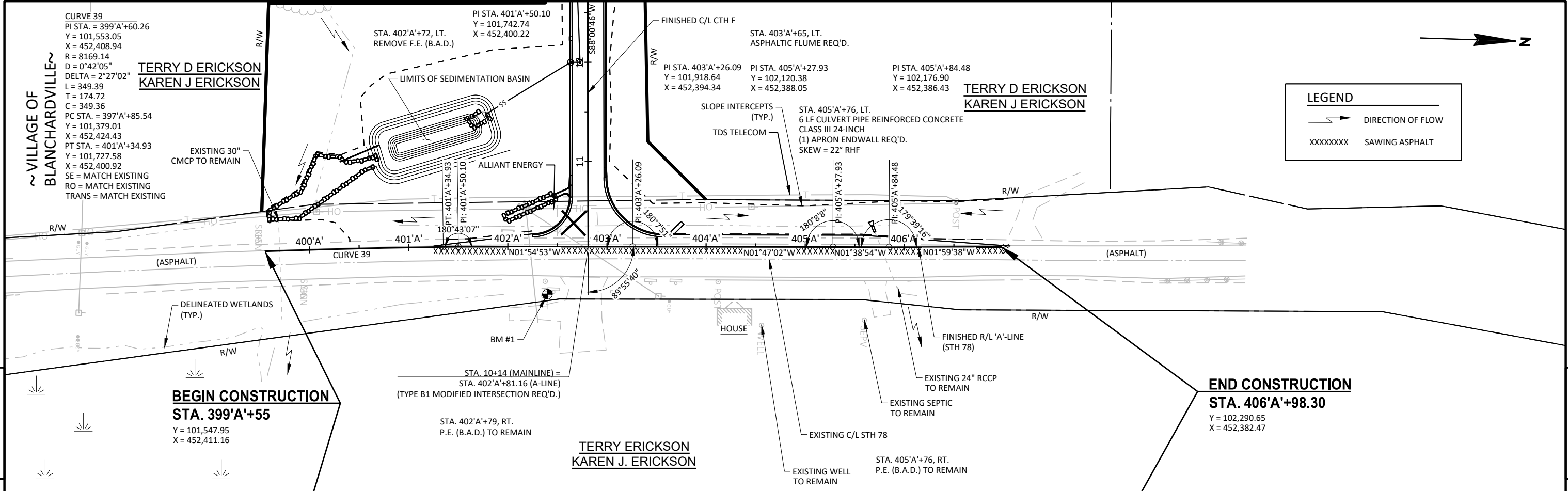
PROJECT NO: 5034-00-72 HWY: CTH F COUNTY: IOWA PLAN AND PROFILE: MAINLINE SHEET: E



BENCH MARKS			
NO.	STA.	DESCRIPTION	ELEV.
34	261+82.30	3/4" IRON REBAR SET, 24.5' RT.	1093.94
35	406'J'+52.76	3/4" IRON REBAR SET, 50.7' LT.	1095.86

EARTHWORK SUMMARY
SEE MISCELLANEOUS QUANTITIES SHEET FOR EARTHWORK SUMMARY

PROJECT NO: 5034-00-72 HWY: CTH F COUNTY: IOWA PLAN AND PROFILE: MAINLINE SHEET: E

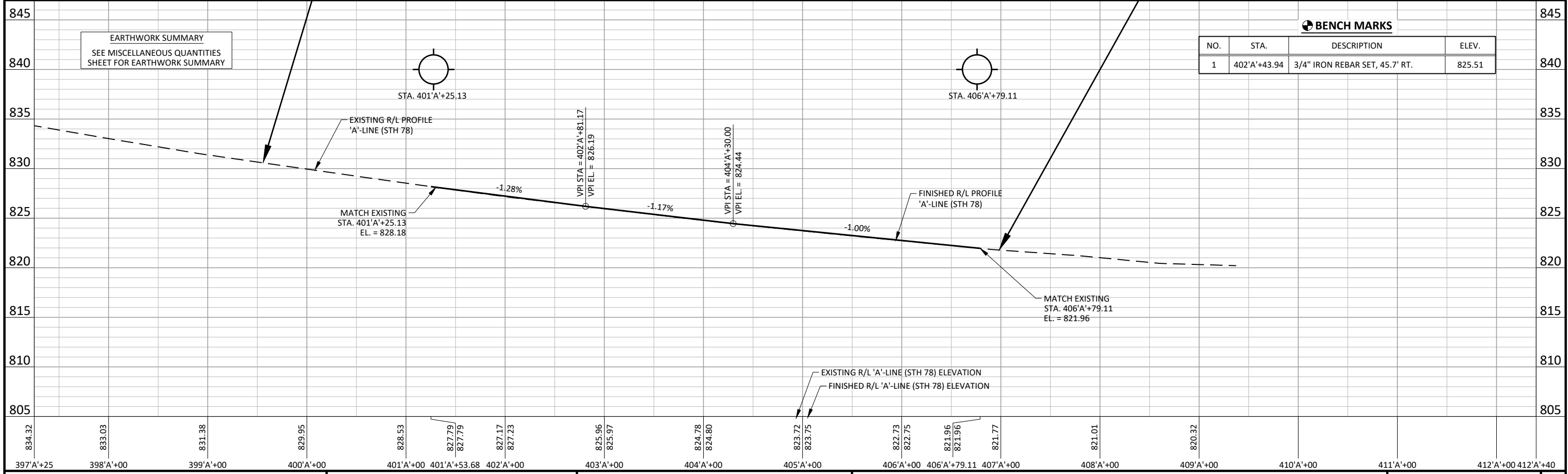


LEGEND

- DIRECTION OF FLOW
- SAWING ASPHALT

BEGIN CONSTRUCTION
STA. 399'A'+55
 Y = 101,547.95
 X = 452,411.16

END CONSTRUCTION
STA. 406'A'+98.30
 Y = 102,290.65
 X = 452,382.47

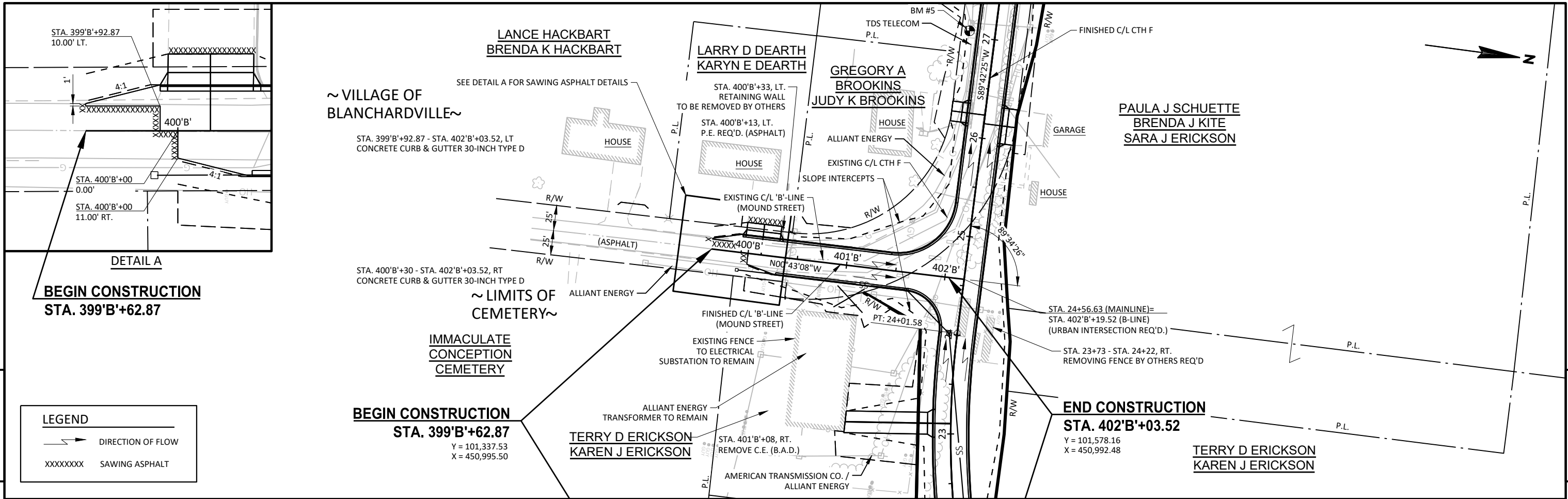


BENCH MARKS

NO.	STA.	DESCRIPTION	ELEV.
1	402'A'+43.94	3/4" IRON REBAR SET, 45.7' RT.	825.51

EARTHWORK SUMMARY
 SEE MISCELLANEOUS QUANTITIES SHEET FOR EARTHWORK SUMMARY

PROJECT NO: 5034-00-72 HWY: CTH F COUNTY: IOWA PLAN AND PROFILE: 'A'-LINE (STH 78) SHEET: 5

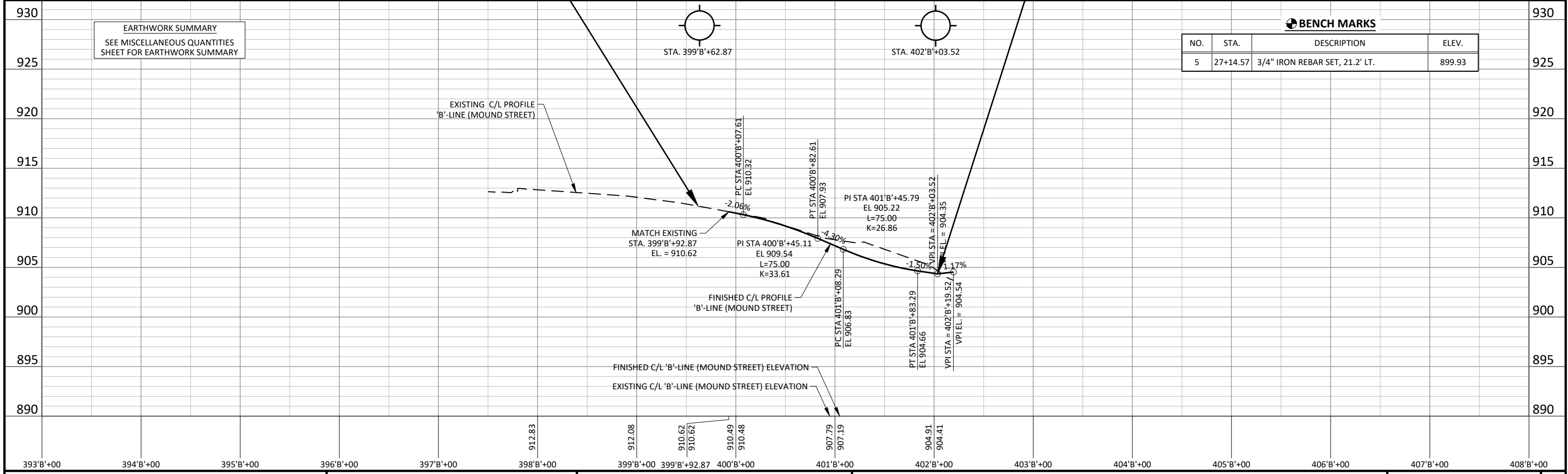


LEGEND

- DIRECTION OF FLOW
- SAWING ASPHALT

5

5

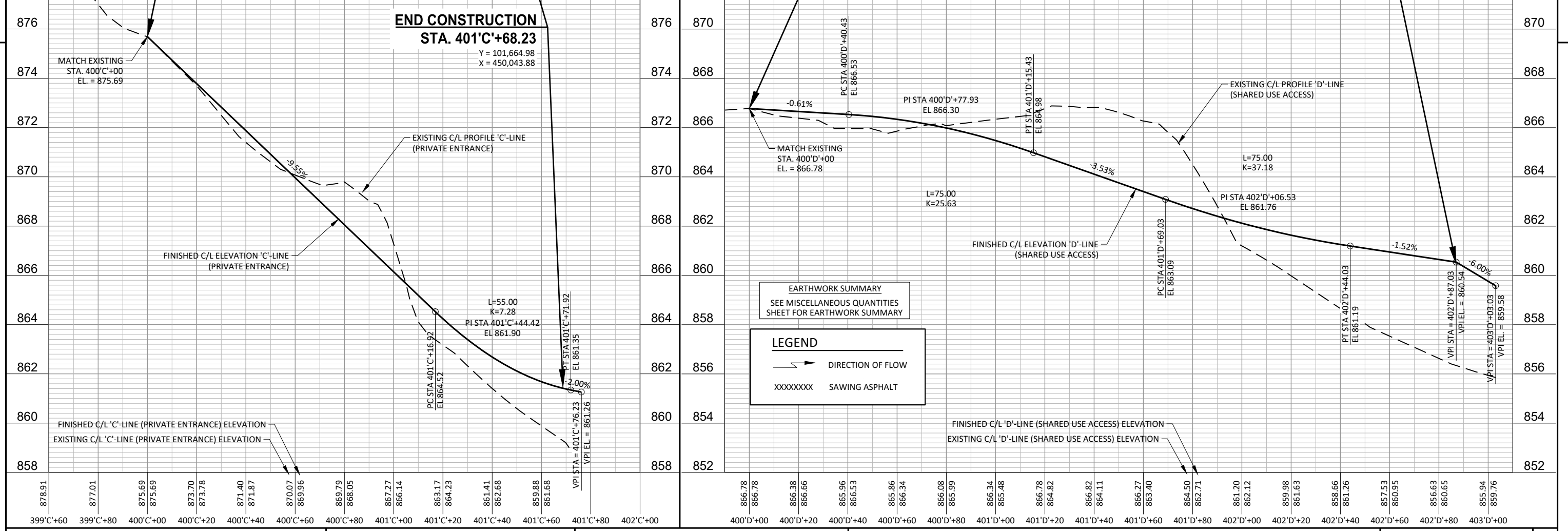
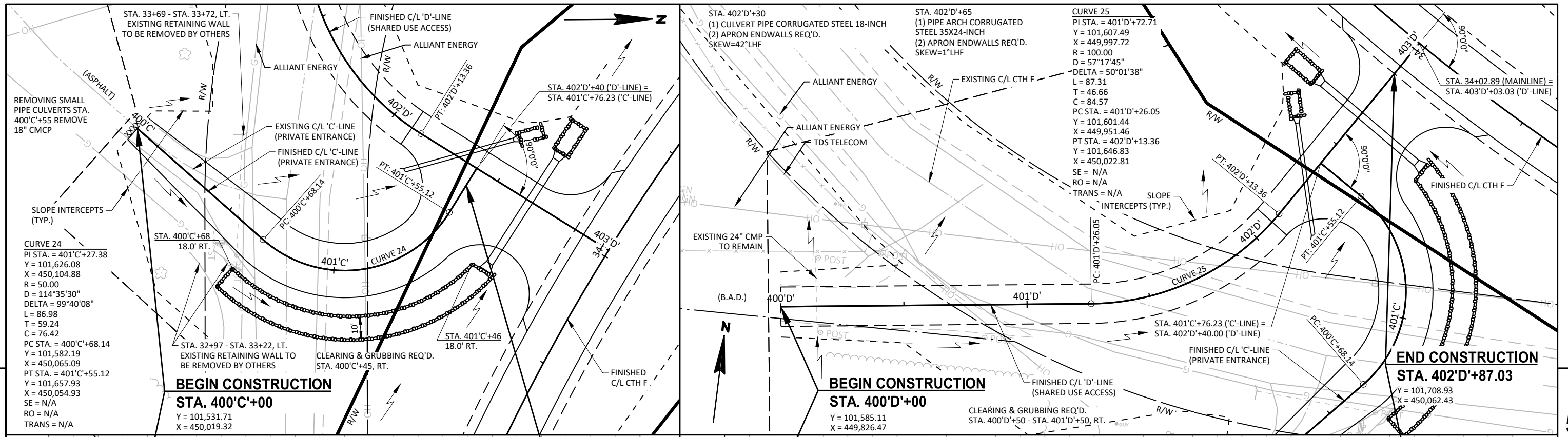


EARTHWORK SUMMARY
SEE MISCELLANEOUS QUANTITIES SHEET FOR EARTHWORK SUMMARY

BENCH MARKS

NO.	STA.	DESCRIPTION	ELEV.
5	27+14.57	3/4" IRON REBAR SET, 21.2' LT.	899.93

PROJECT NO: 5034-00-72 HWY: CTH F COUNTY: IOWA PLAN AND PROFILE: 'B'-LINE (MOUND STREET) SHEET: E

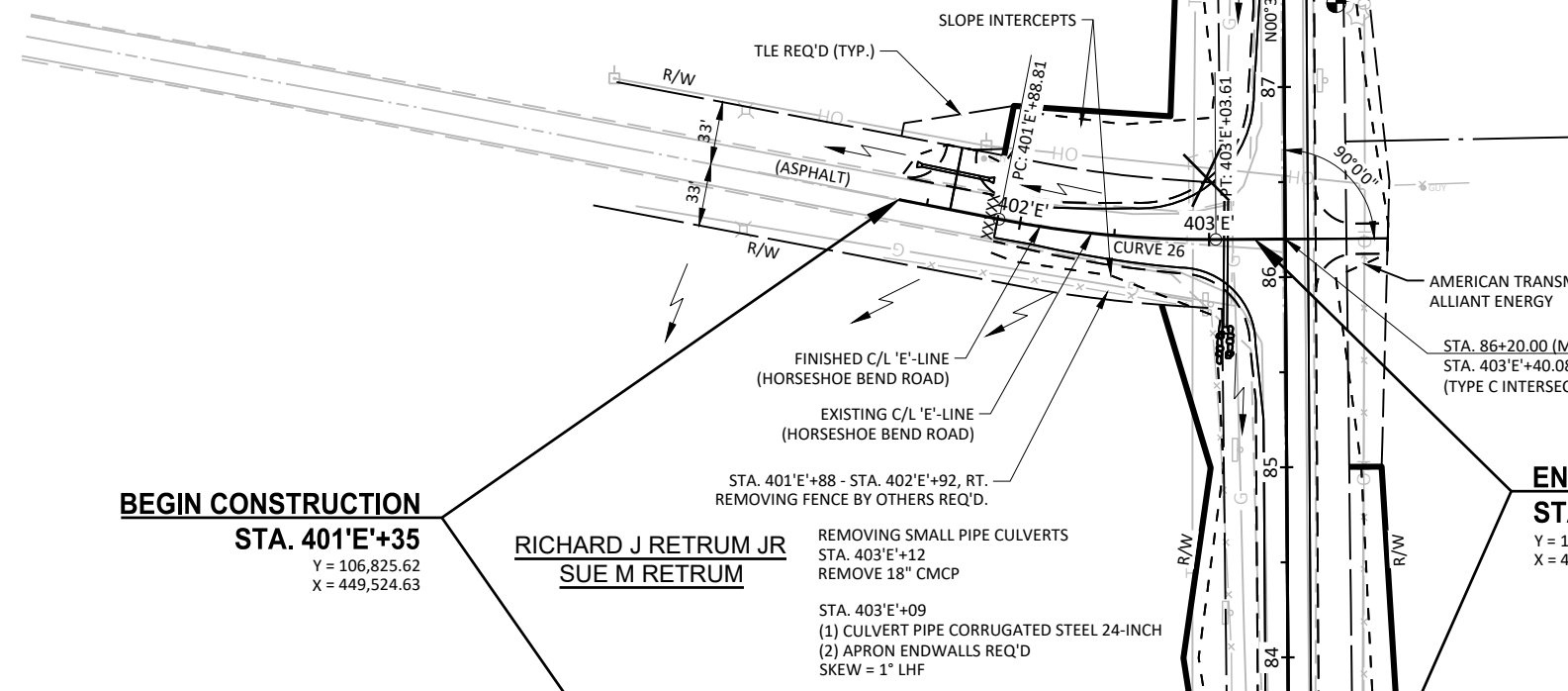


RICHARD J RETRUM JR
SUE M RETRUM

STEVEN S ZIMMERMAN
STEPHANIE J ZIMMERMAN



STA. 402'E'+98, LT. RELOCATE F.E. (B.A.D.)
STA. 401'E'+63, LT. CONSTRUCT F.E. (ASPHALT)
(1) CULVERT PIPE CORRUGATED STEEL 18-INCH
(2) APRON ENDWALLS REQ'D



CURVE 26
PI STA. = 402'E'+46.40
Y = 106,804.77
X = 449,634.06
R = 575.00
D = 9°57'52"
DELTA = 11°26'20"
L = 114.80
T = 57.59
C = 114.61
PC STA. = 401'E'+88.81
Y = 106,815.55
X = 449,577.49
PT STA. = 403'E'+03.61
Y = 106,805.42
X = 449,691.65
SE = N/A
RO = N/A
TRANS = N/A

BEGIN CONSTRUCTION
STA. 401'E'+35
Y = 106,825.62
X = 449,524.63

RICHARD J RETRUM JR
SUE M RETRUM

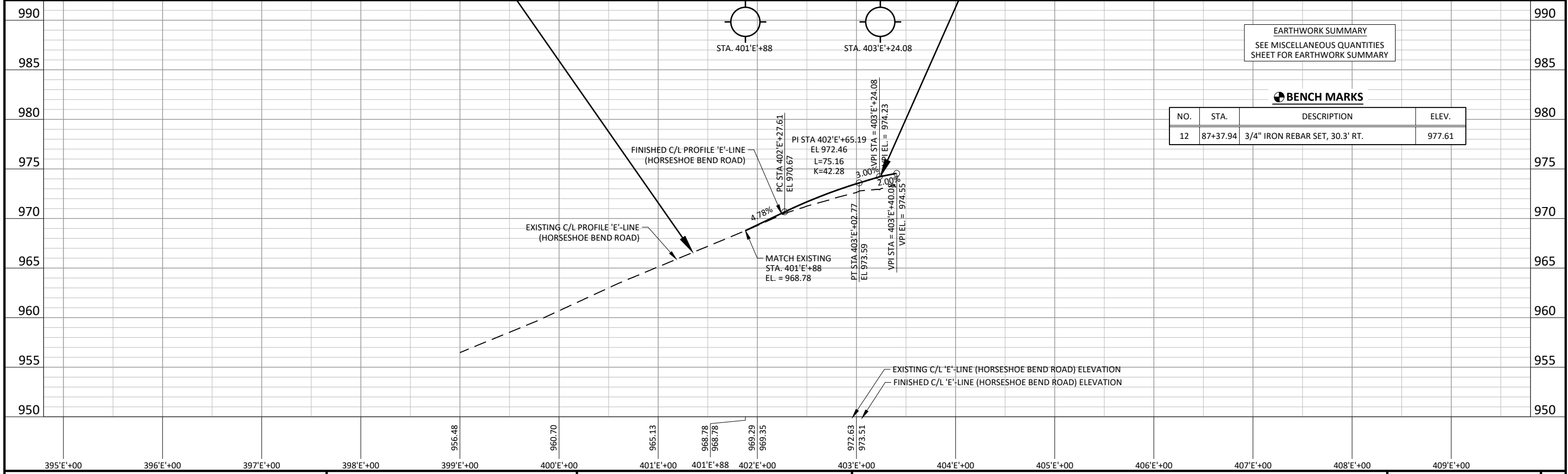
REMOVING SMALL PIPE CULVERTS
STA. 403'E'+12
REMOVE 18" CMCP
STA. 403'E'+09
(1) CULVERT PIPE CORRUGATED STEEL 24-INCH
(2) APRON ENDWALLS REQ'D
SKEW = 1° LHF

END CONSTRUCTION
STA. 403'E'+24.08
Y = 106,805.66
X = 449,712.12

RICHARD D ZACHMAN REV TRUST

LEGEND

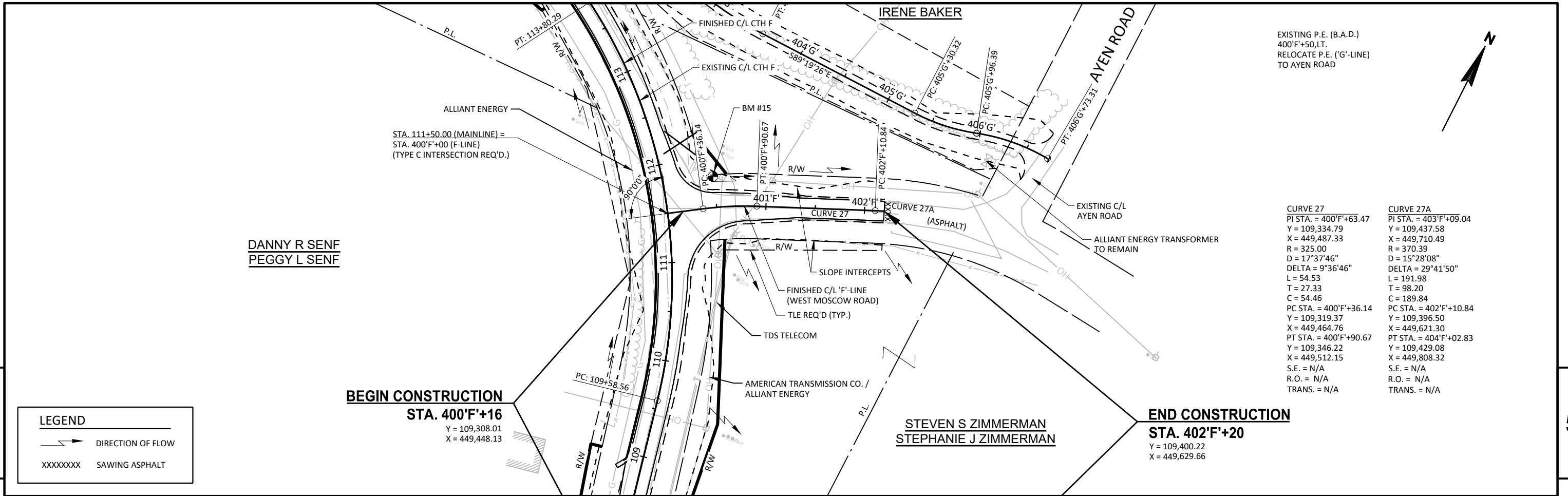
- DIRECTION OF FLOW
- SAWING ASPHALT



EARTHWORK SUMMARY
SEE MISCELLANEOUS QUANTITIES SHEET FOR EARTHWORK SUMMARY

BENCH MARKS

NO.	STA.	DESCRIPTION	ELEV.
12	87+37.94	3/4" IRON REBAR SET, 30.3' RT.	977.61



EXISTING P.E. (B.A.D.)
400'F'+50.LT.
RELOCATE P.E. ('G'-LINE)
TO AYEN ROAD

CURVE 27		CURVE 27A	
PI STA. = 400'F'+63.47	Y = 109,334.79	PI STA. = 403'F'+09.04	Y = 109,437.58
X = 449,487.33	R = 325.00	X = 449,710.49	R = 370.39
D = 17°37'46"	DELTA = 9°36'46"	L = 191.98	DELTA = 29°41'50"
L = 54.53	T = 27.33	L = 191.98	T = 98.20
C = 54.46	PC STA. = 400'F'+36.14	C = 189.84	PC STA. = 402'F'+10.84
Y = 109,319.37	X = 449,464.76	Y = 109,396.50	X = 449,621.30
PT STA. = 400'F'+90.67	Y = 109,346.22	PT STA. = 404'F'+02.83	Y = 109,429.08
X = 449,512.15	S.E. = N/A	X = 449,808.32	S.E. = N/A
R.O. = N/A	TRANS. = N/A	R.O. = N/A	TRANS. = N/A

BEGIN CONSTRUCTION

STA. 400'F'+16

Y = 109,308.01
X = 449,448.13

END CONSTRUCTION

STA. 402'F'+20

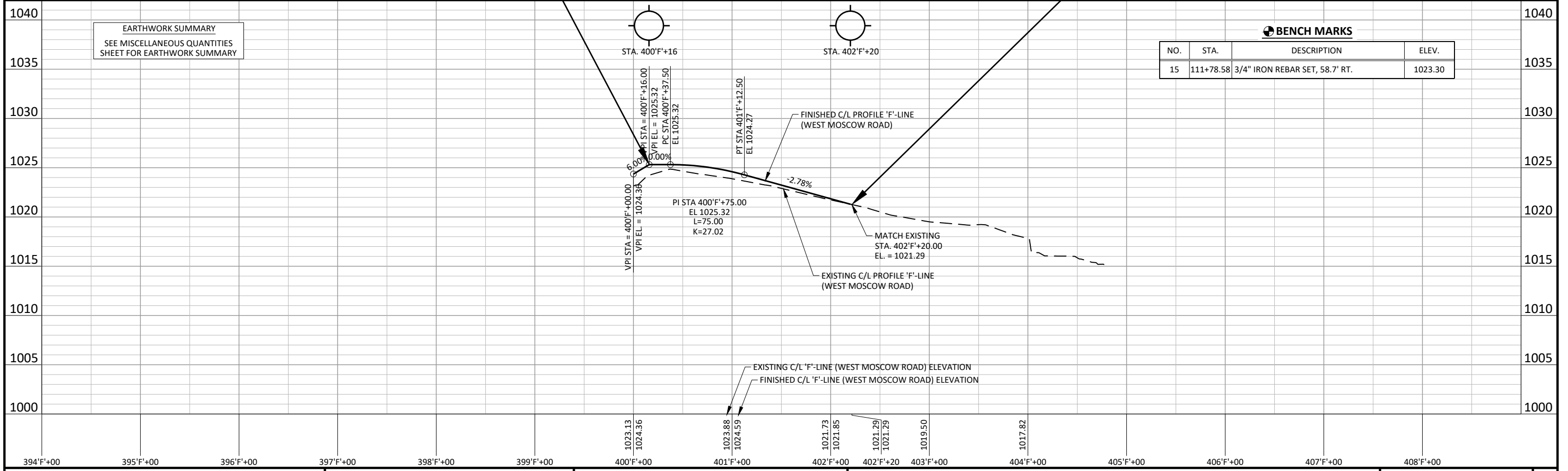
Y = 109,400.22
X = 449,629.66

LEGEND

	DIRECTION OF FLOW
XXXXXXX	SAWING ASPHALT

5

5



EARTHWORK SUMMARY
SEE MISCELLANEOUS QUANTITIES
SHEET FOR EARTHWORK SUMMARY

BENCH MARKS

NO.	STA.	DESCRIPTION	ELEV.
15	111+78.58	3/4" IRON REBAR SET, 58.7' RT.	1023.30

PROJECT NO: 5034-00-72 HWY: CTH F COUNTY: IOWA PLAN AND PROFILE: 'F'-LINE (WEST MOSCOW ROAD) SHEET: E

LEGEND

- DIRECTION OF FLOW
- SAWING ASPHALT
- EXISTING P.E. TO BE REMOVED

CURVE 28	CURVE 33	CURVE 34	CURVE 35
PI STA. = 400'G'+83.64	PI STA. = 402'G'+69.56	PI STA. = 405'G'+63.64	PI STA. = 406'G'+35.33
Y = 109,598.01	Y = 109,506.04	Y = 109,502.52	Y = 109,525.15
X = 449,183.19	X = 449,348.05	X = 449,645.85	X = 449,714.52
R = 166.82	R = 357.04	R = 200.00	R = 200.00
D = 34°20'48"	D = 16°02'52"	D = 28°38'52"	D = 28°38'52"
DELTA = 33°24'34"	DELTA = 28°28'45"	DELTA = 18°54'50"	DELTA = 22°02'04"
L = 97.27	L = 177.47	L = 66.02	L = 76.92
T = 50.06	T = 90.61	T = 33.31	T = 38.94
C = 95.90	C = 175.65	C = 65.72	C = 76.44
PC STA. = 400'G'+33.57	PC STA. = 401'G'+78.96	PC STA. = 405'G'+30.32	PC STA. = 405'G'+96.39
Y = 109,642.44	Y = 109,550.18	Y = 109,502.92	Y = 109,512.96
X = 449,160.13	X = 449,268.93	X = 449,612.54	X = 449,677.54
PT STA. = 401'G'+30.84	PT STA. = 403'G'+56.42	PT STA. = 405'G'+96.34	PT STA. = 406'G'+73.31
Y = 109,573.62	Y = 109,504.97	Y = 109,512.95	Y = 109,522.57
X = 449,226.91	X = 449,438.66	X = 449,677.49	X = 449,753.37
S.E. = N/A	S.E. = N/A	S.E. = N/A	S.E. = N/A
R.O. = N/A	R.O. = N/A	R.O. = N/A	R.O. = N/A
TRANS. = N/A	TRANS. = N/A	TRANS. = N/A	TRANS. = N/A

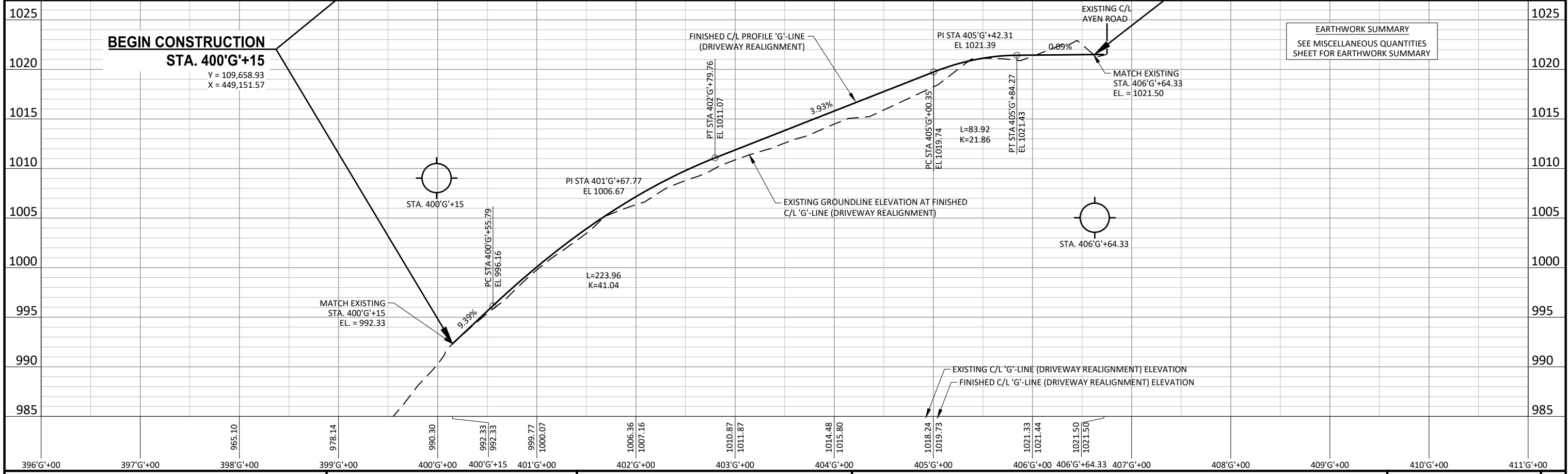
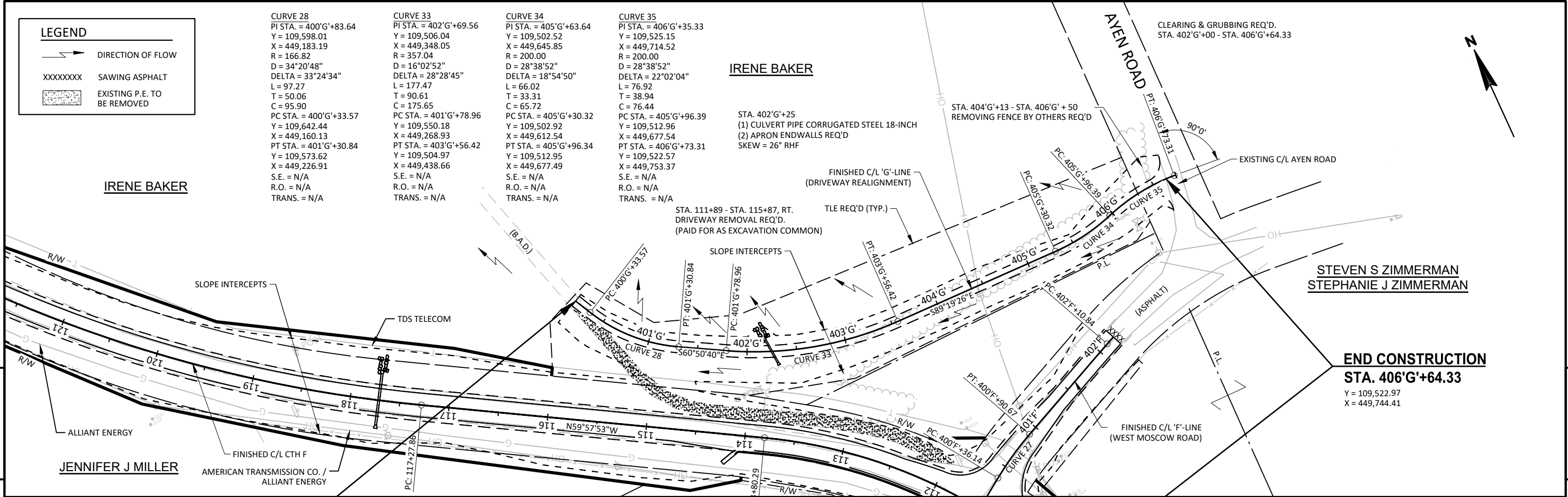
IRENE BAKER

STA. 402'G'+25
 (1) CULVERT PIPE CORRUGATED STEEL 18-INCH
 (2) APRON ENDWALLS REQ'D
 SKEW = 26° RHF

CLEARING & GRUBBING REQ'D.
 STA. 402'G'+00 - STA. 406'G'+64.33

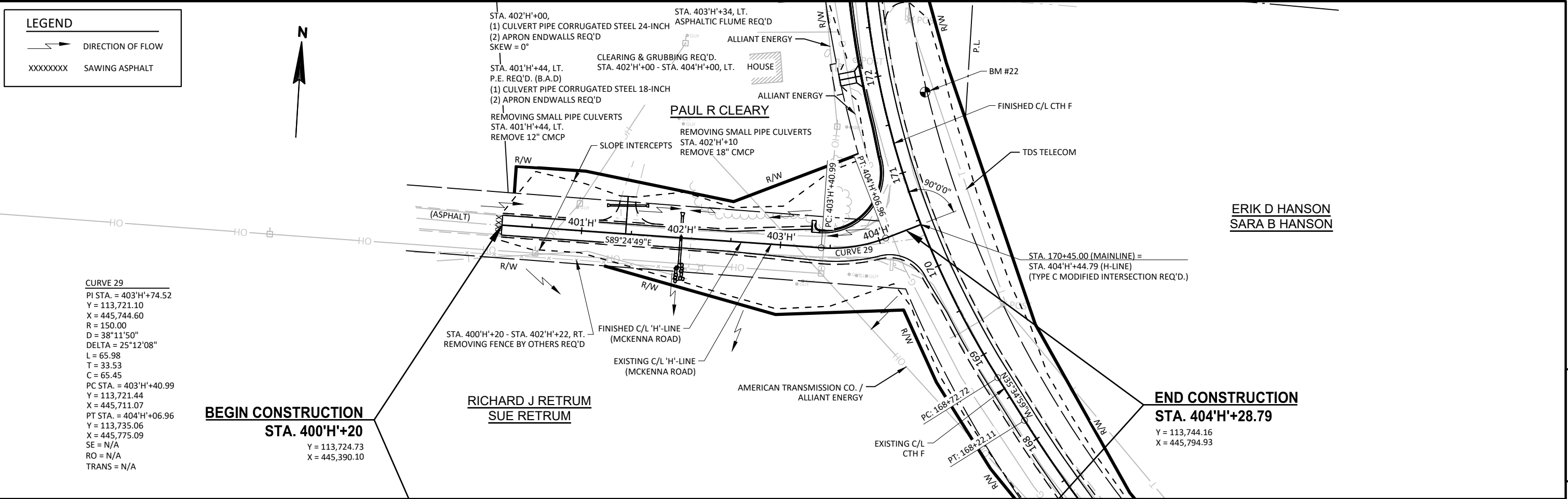
STEVEN S ZIMMERMAN
STEPHANIE J ZIMMERMAN

END CONSTRUCTION
STA. 406'G'+64.33
 Y = 109,522.97
 X = 449,744.41



EARTHWORK SUMMARY
 SEE MISCELLANEOUS QUANTITIES SHEET FOR EARTHWORK SUMMARY

PROJECT NO: 5034-00-72	HWY: CTH F	COUNTY: IOWA	PLAN AND PROFILE: 'G'-LINE (DRIVEWAY REALIGNMENT)	SHEET	E
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CURVE 29
 PI STA. = 403'H'+74.52
 Y = 113,721.10
 X = 445,744.60
 R = 150.00
 D = 38°11'50"
 DELTA = 25°12'08"
 L = 65.98
 T = 33.53
 C = 65.45
 PC STA. = 403'H'+40.99
 Y = 113,721.44
 X = 445,711.07
 PT STA. = 404'H'+06.96
 Y = 113,735.06
 X = 445,775.09
 SE = N/A
 RO = N/A
 TRANS = N/A

BEGIN CONSTRUCTION
STA. 400'H'+20
 Y = 113,724.73
 X = 445,390.10

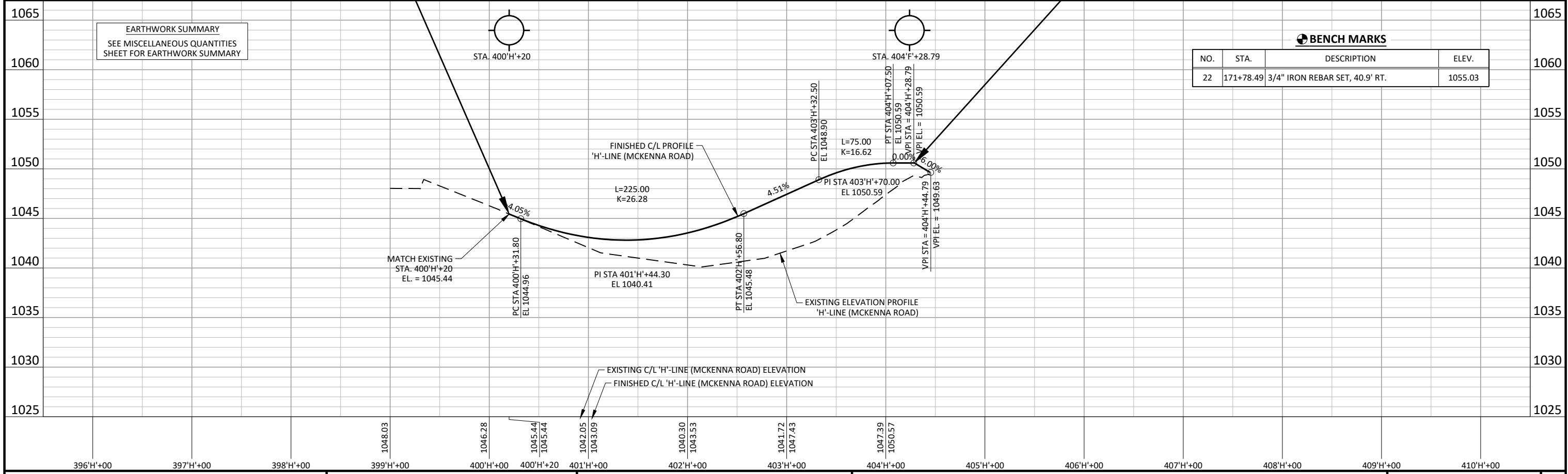
END CONSTRUCTION
STA. 404'H'+28.79
 Y = 113,744.16
 X = 445,794.93

ERIK D HANSON
 SARA B HANSON

EARTHWORK SUMMARY
 SEE MISCELLANEOUS QUANTITIES
 SHEET FOR EARTHWORK SUMMARY

BENCH MARKS

NO.	STA.	DESCRIPTION	ELEV.
22	171+78.49	3/4" IRON REBAR SET, 40.9' RT.	1055.03



PROJECT NO: 5034-00-72 HWY: CTH F COUNTY: IOWA PLAN AND PROFILE: 'H'-LINE (MCKENNA ROAD) SHEET: E

LEGEND

→ DIRECTION OF FLOW

XXXXXXX SAWING ASPHALT

CHARLES L HARTLEY JR
TRACEY L HARTLEY

REMOVING SMALL PIPE CULVERTS
STA. 405'+10
REMOVE 18" CMCP

FRONTIER COMMUNICATIONS

FINISHED C/L 'I'-LINE
(EAST PECATONICA ROAD)

STA. 238+45.00 (MAINLINE) =
STA. 409'+40.87 ('I'-LINE)
(TYPE C INTERSECTION REQ'D.)

JAMES MCGHEE
MALA MCGHEE

RUNDEN PROPERTIES, LLC

CURVE 31
PI STA. = 402'+26.81
Y = 120,190.48
X = 443,656.01
R = 2000.00
D = 2°51'53"
DELTA = 4°30'21"
L = 157.28
T = 78.68
C = 157.24
PC STA. = 401'+48.13
Y = 120,184.42
X = 443,577.56
PT STA. = 403'+05.41
Y = 120,190.36
X = 443,734.69
SE = N/A
RO = N/A
TRANS = N/A

BEGIN CONSTRUCTION
STA. 401'+70
Y = 120,185.98
X = 443,599.38

SIGG LIVING TRUST

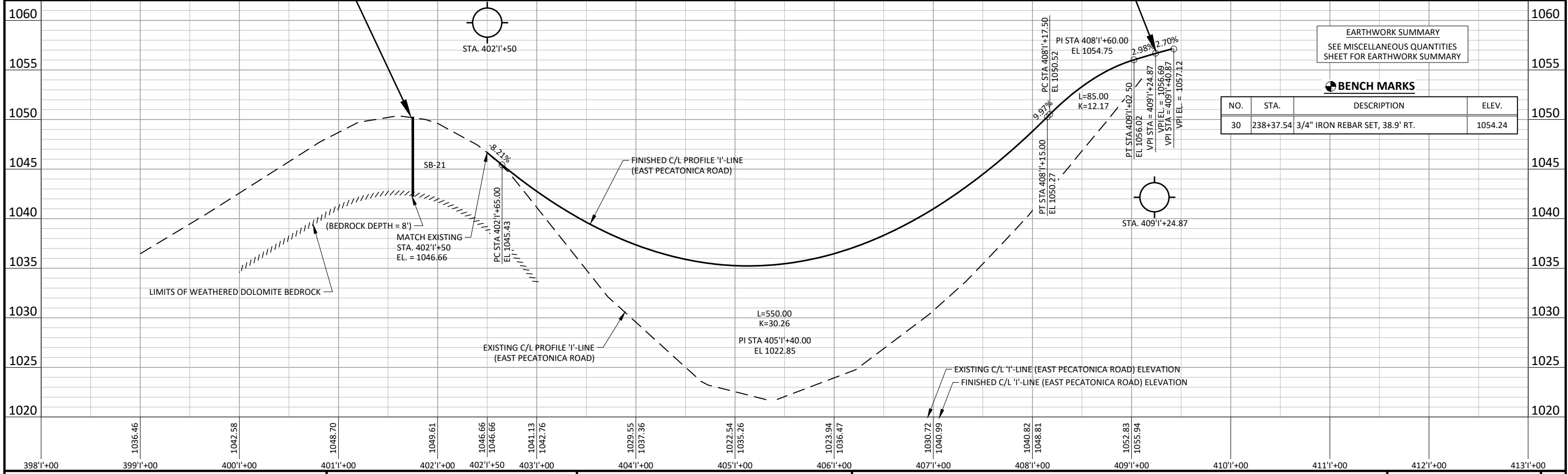
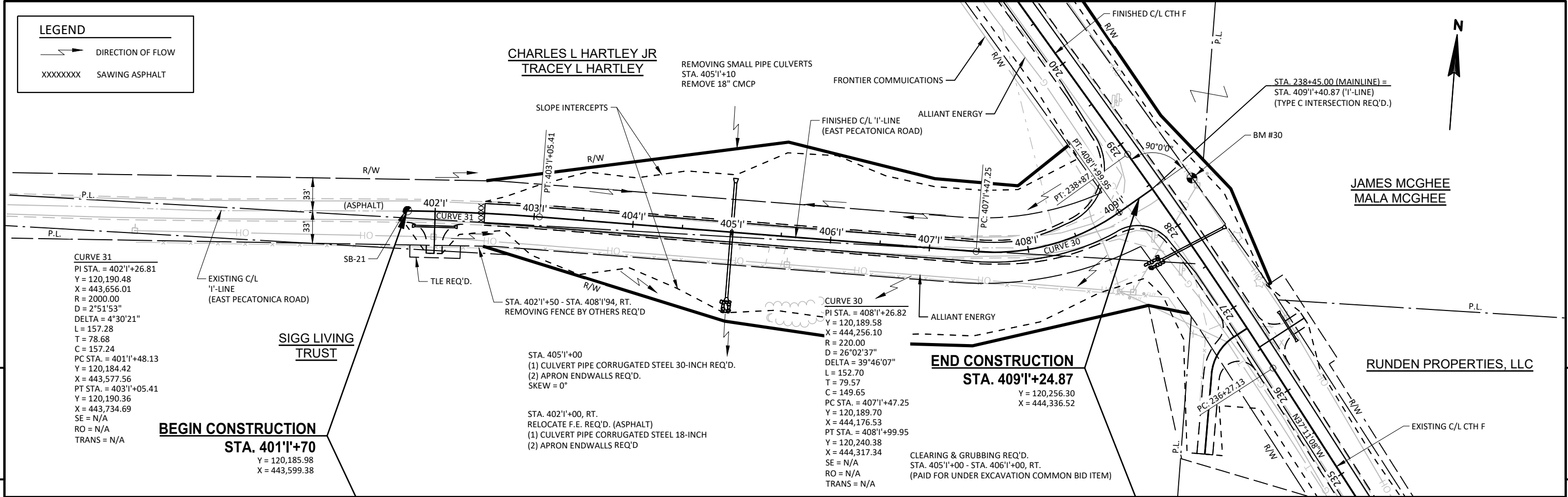
STA. 405'+00
(1) CULVERT PIPE CORRUGATED STEEL 30-INCH REQ'D.
(2) APRON ENDWALLS REQ'D.
SKEW = 0°

STA. 402'+00, RT.
RELOCATE F.E. REQ'D. (ASPHALT)
(1) CULVERT PIPE CORRUGATED STEEL 18-INCH
(2) APRON ENDWALLS REQ'D

END CONSTRUCTION
STA. 409'+24.87
Y = 120,256.30
X = 444,336.52

CLEARING & GRUBBING REQ'D.
STA. 405'+00 - STA. 406'+00, RT.
(PAID FOR UNDER EXCAVATION COMMON BID ITEM)

CURVE 30
PI STA. = 408'+26.82
Y = 120,189.58
X = 444,256.10
R = 220.00
D = 26°02'37"
DELTA = 39°46'07"
L = 152.70
T = 79.57
C = 149.65
PC STA. = 407'+47.25
Y = 120,189.70
X = 444,176.53
PT STA. = 408'+99.95
Y = 120,240.38
X = 444,317.34
SE = N/A
RO = N/A
TRANS = N/A



EARTHWORK SUMMARY
SEE MISCELLANEOUS QUANTITIES SHEET FOR EARTHWORK SUMMARY

BENCH MARKS

NO.	STA.	DESCRIPTION	ELEV.
30	238+37.54	3/4" IRON REBAR SET, 38.9' RT.	1054.24

LEGEND

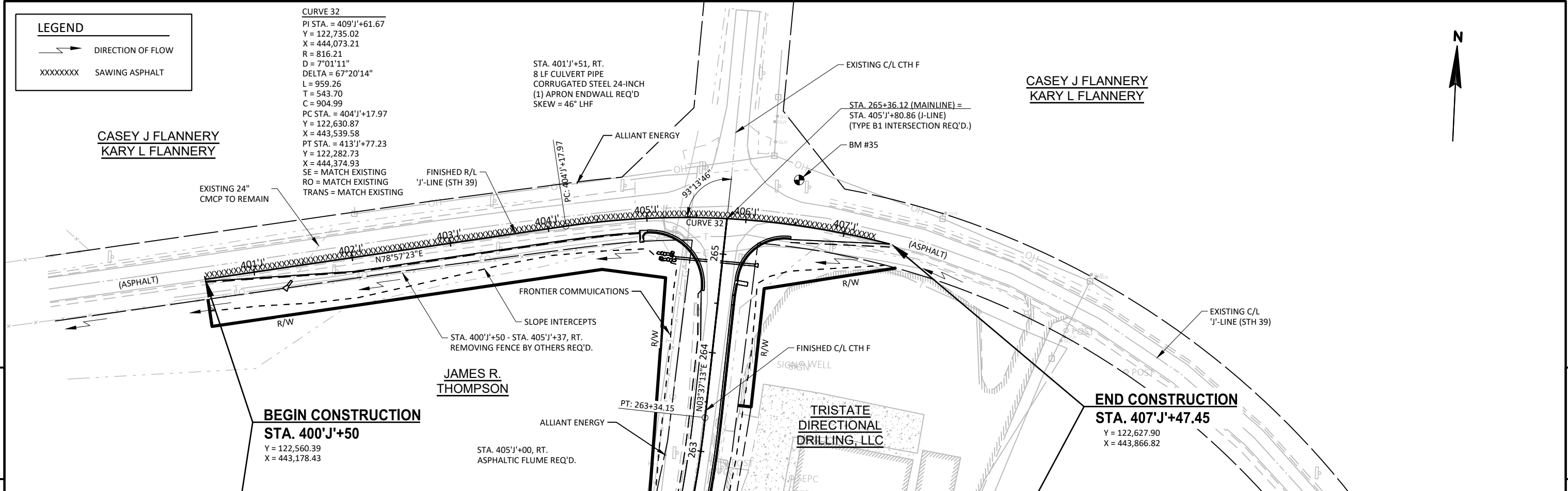
→ DIRECTION OF FLOW

XXXXXXX SAWING ASPHALT

CURVE 32
 PI STA. = 409'+61.67
 Y = 122,735.02
 X = 444,073.21
 R = 816.21
 D = 7°01'11"
 DELTA = 67°20'14"
 L = 959.26
 T = 543.70
 C = 904.99
 PC STA. = 404'+17.97
 Y = 122,630.87
 X = 443,539.58
 PT STA. = 413'+77.23
 Y = 122,282.73
 X = 444,374.93
 SE = MATCH EXISTING
 RO = MATCH EXISTING
 TRANS = MATCH EXISTING

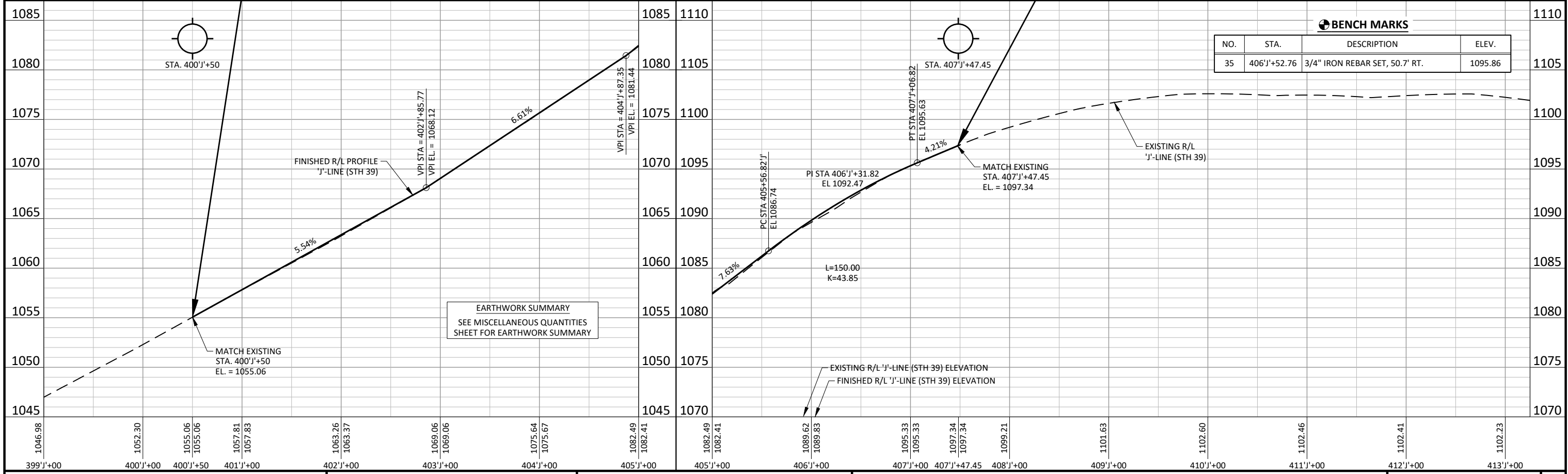
CASEY J FLANNERY
 KARY L FLANNERY

CASEY J FLANNERY
 KARY L FLANNERY



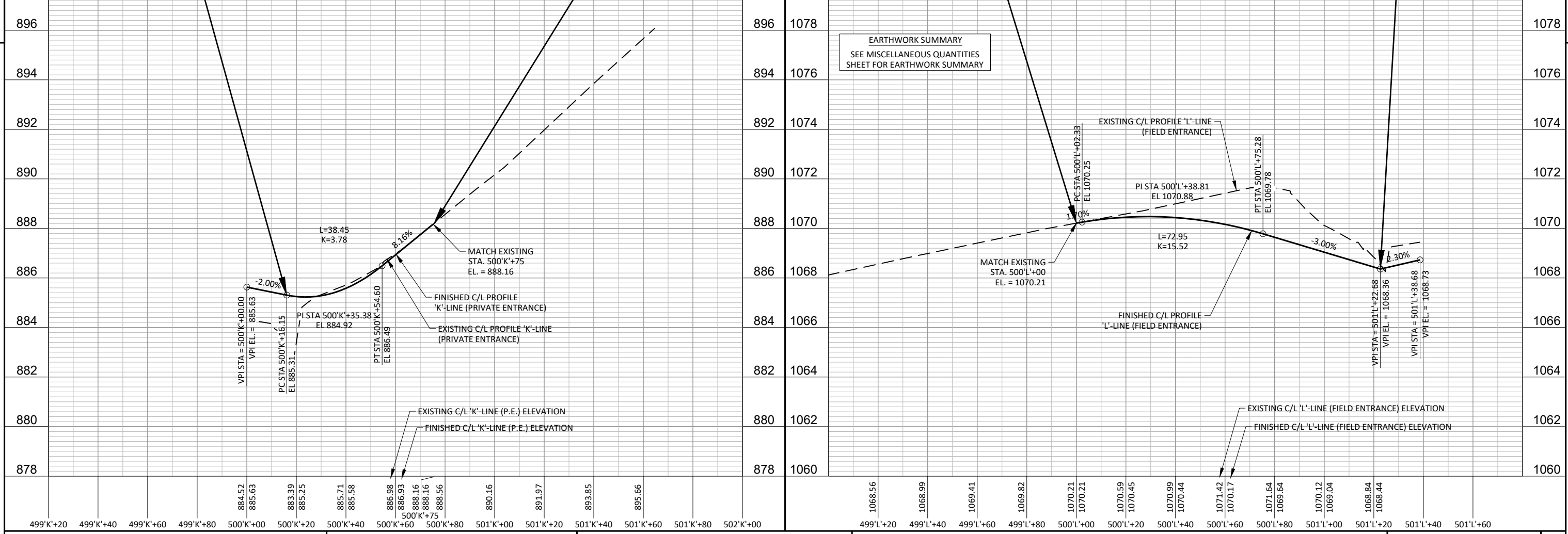
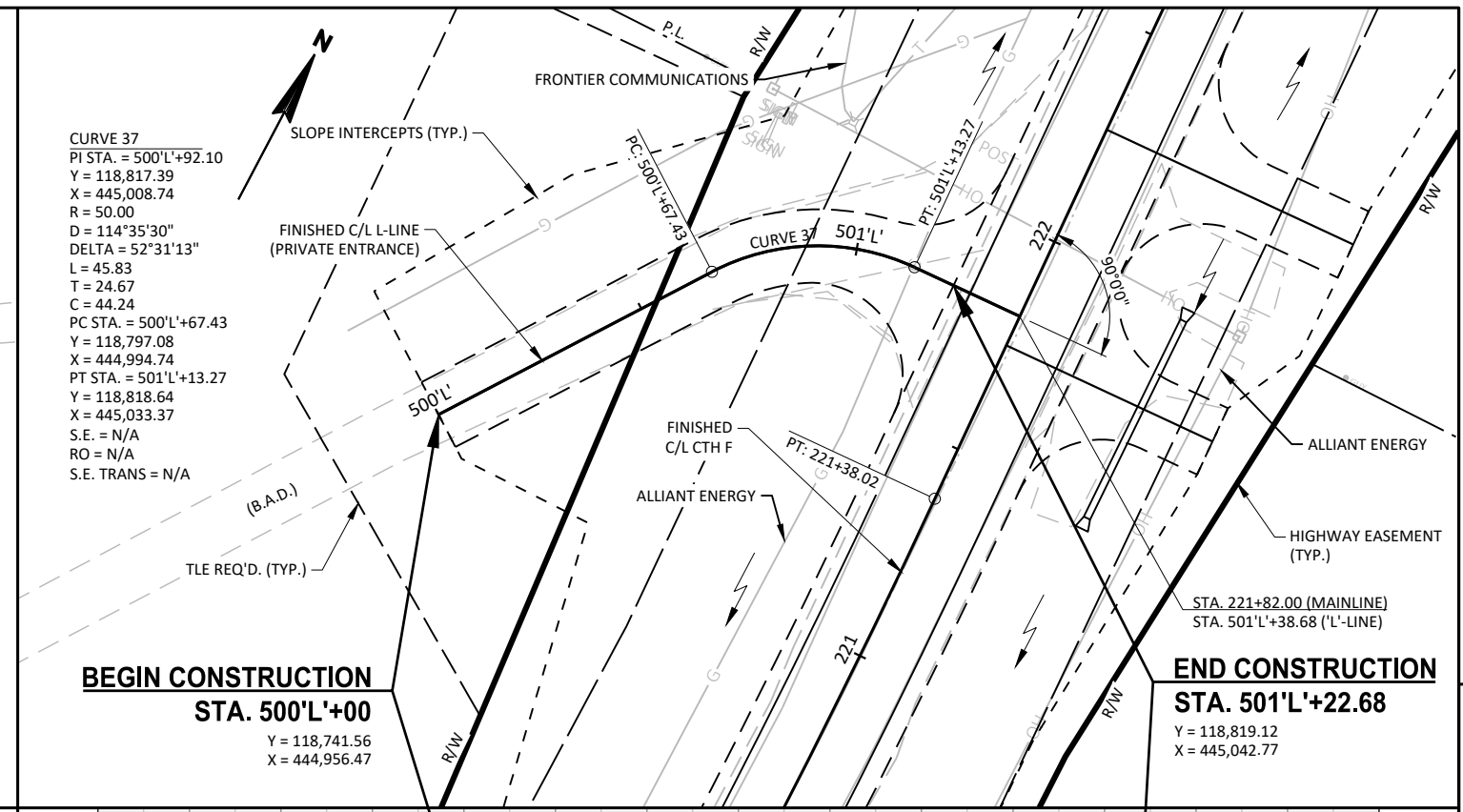
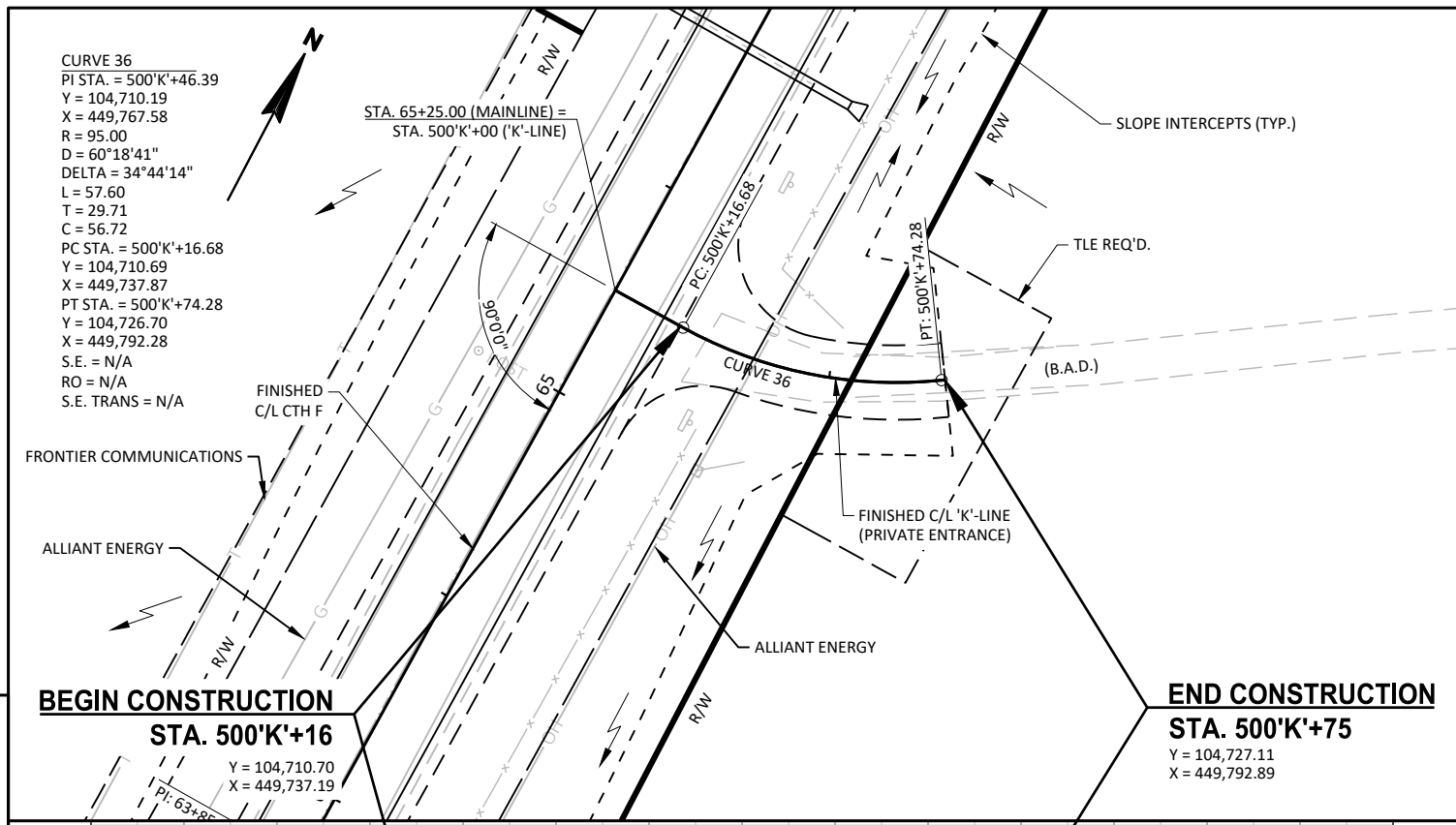
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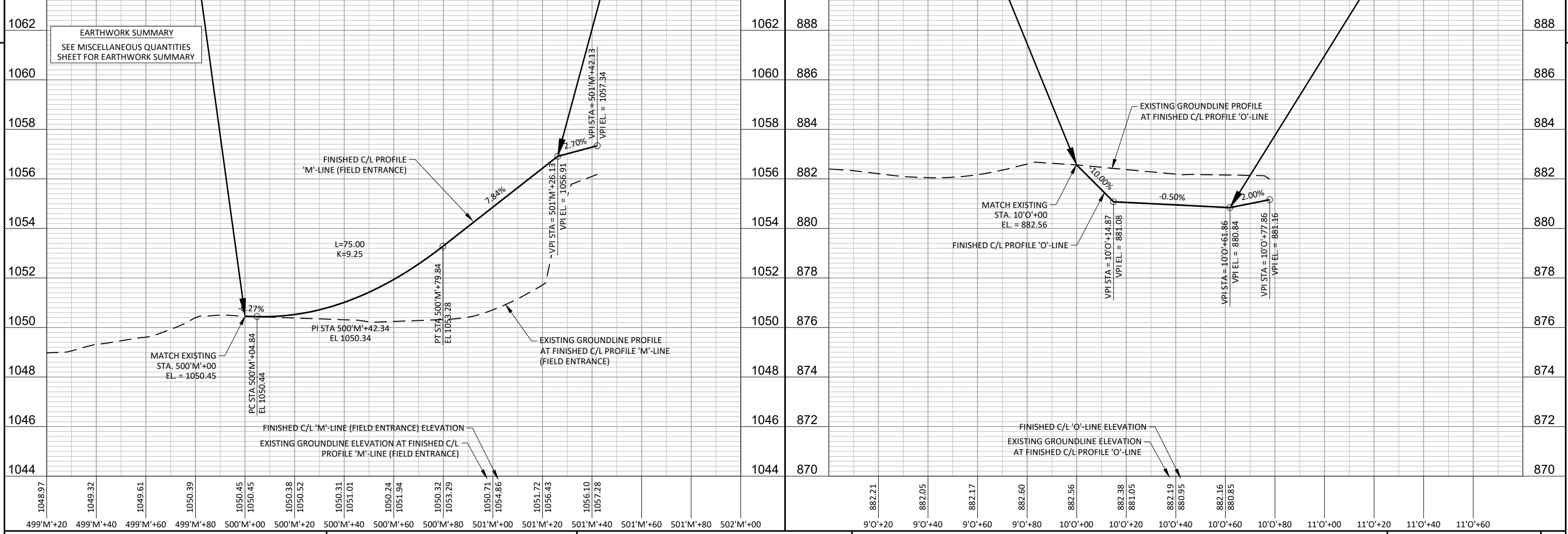
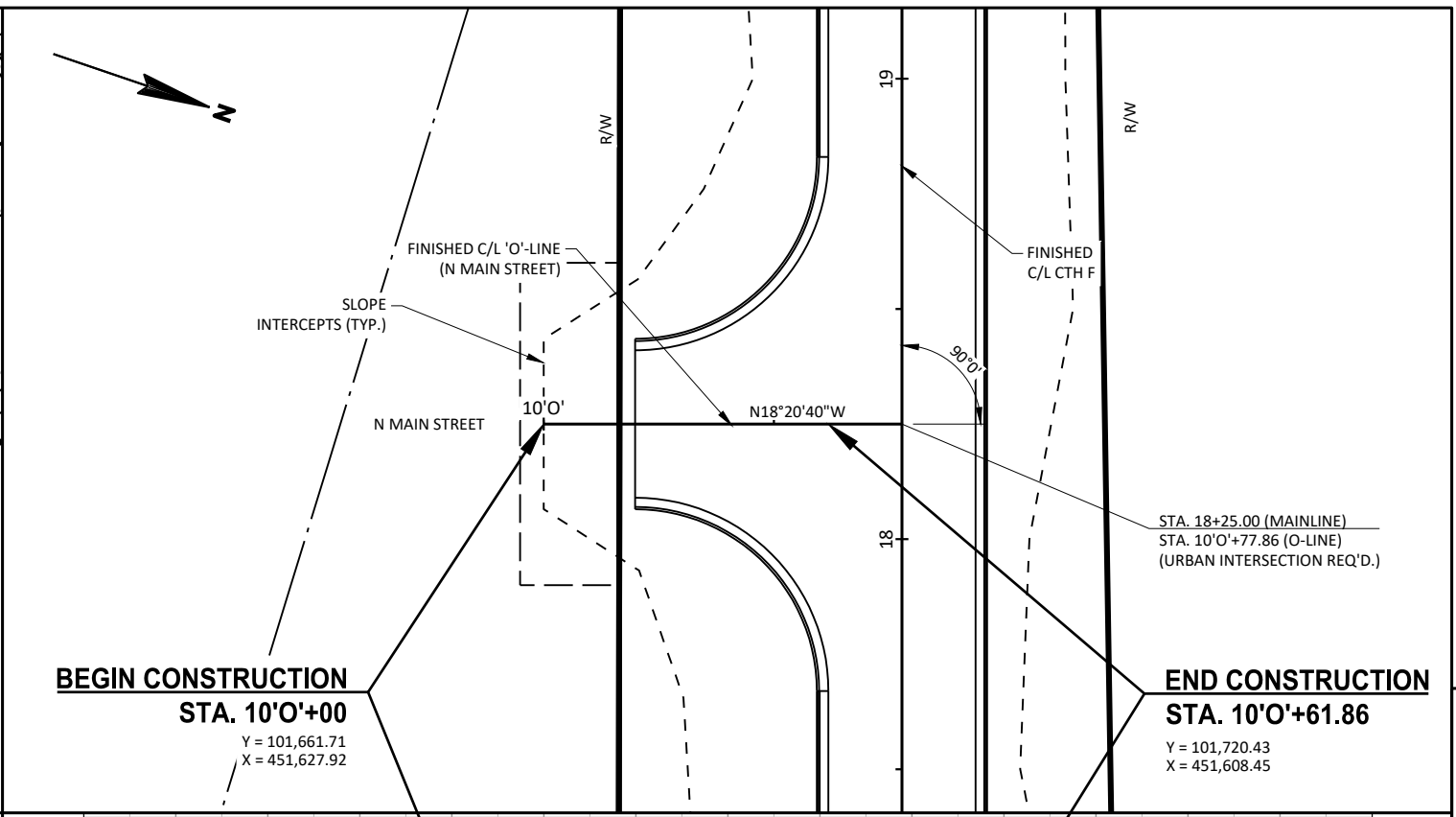
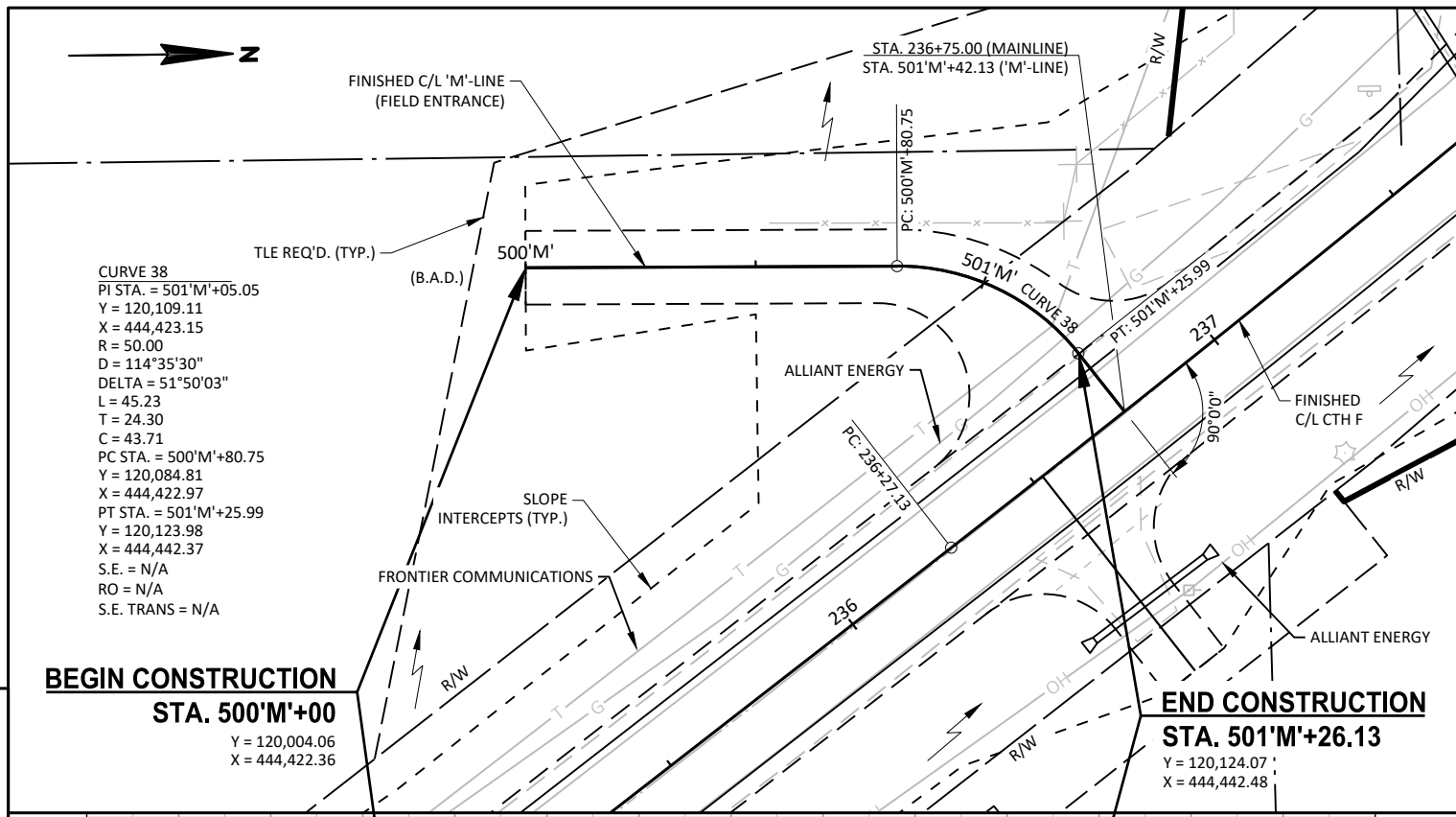
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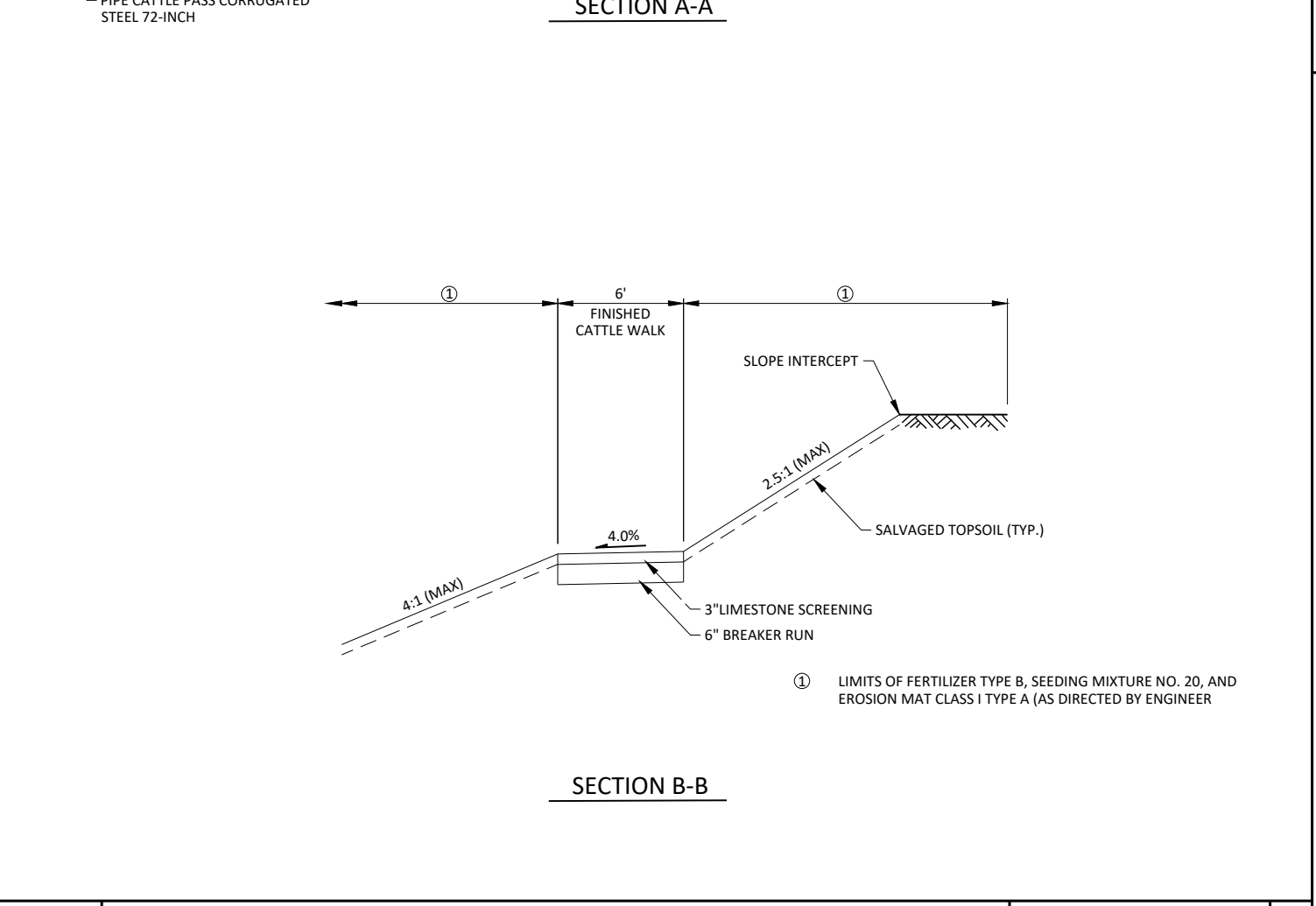
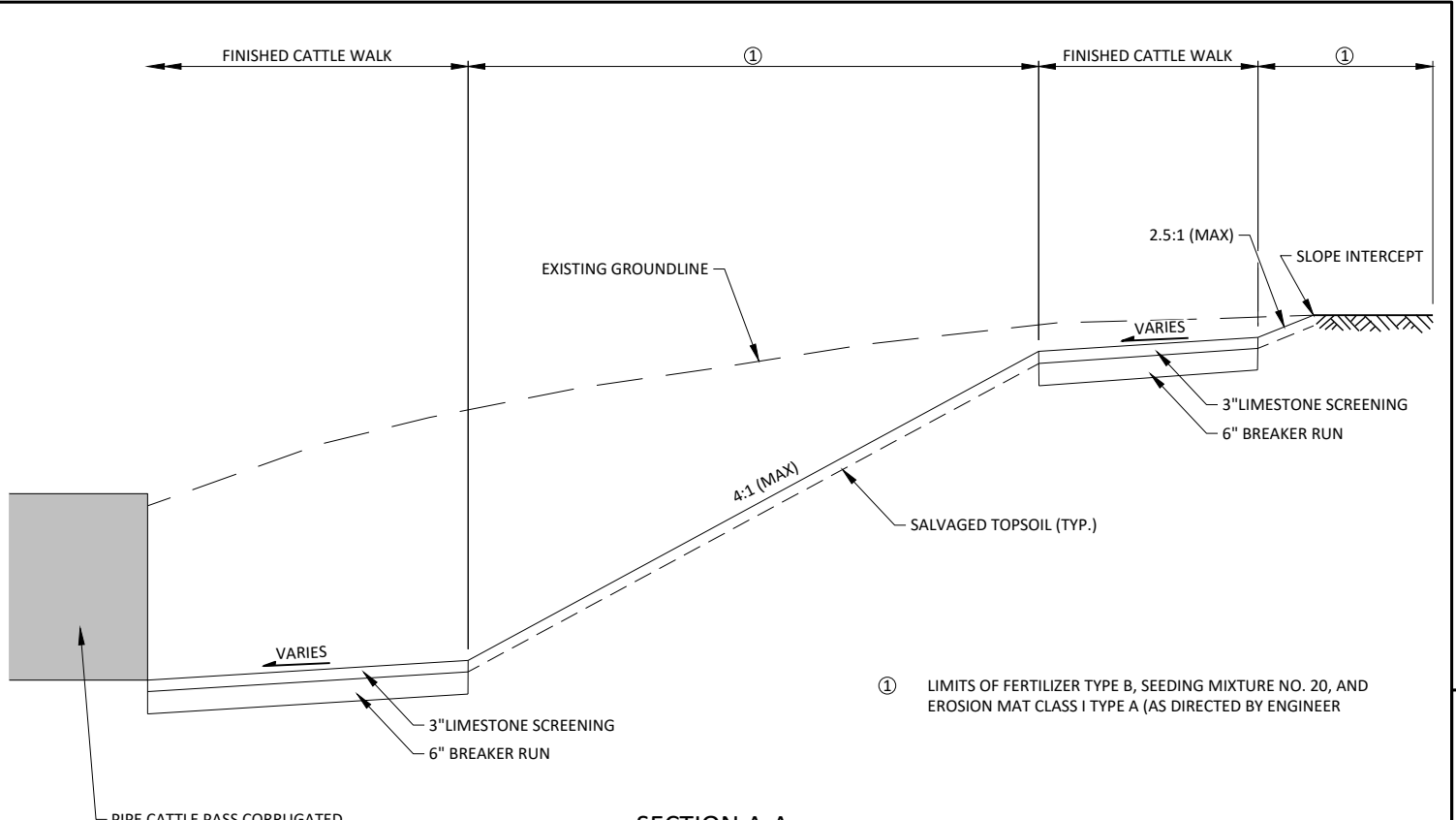
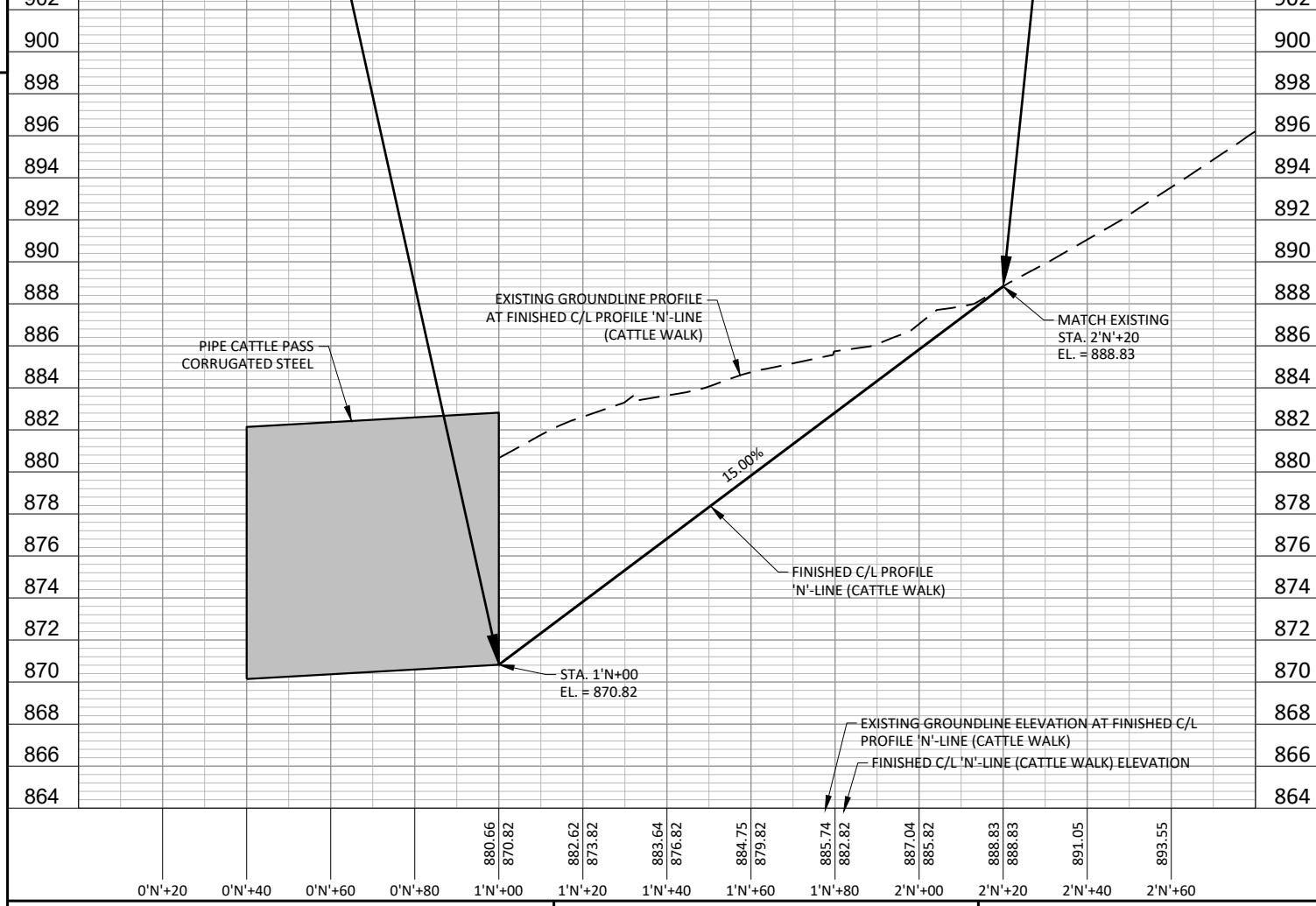
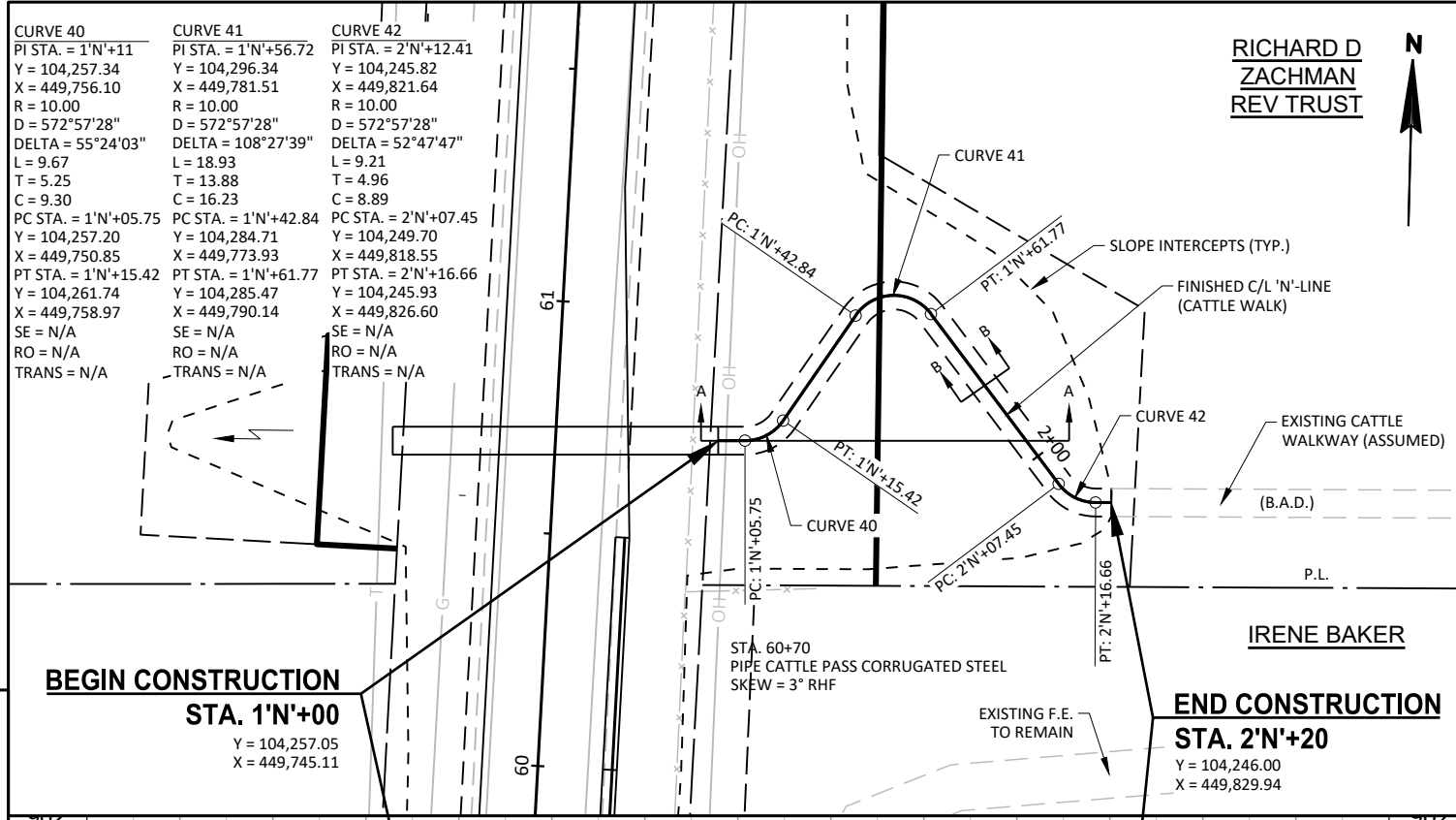
EARTHWORK SUMMARY
 SEE MISCELLANEOUS QUANTITIES SHEET FOR EARTHWORK SUMMARY

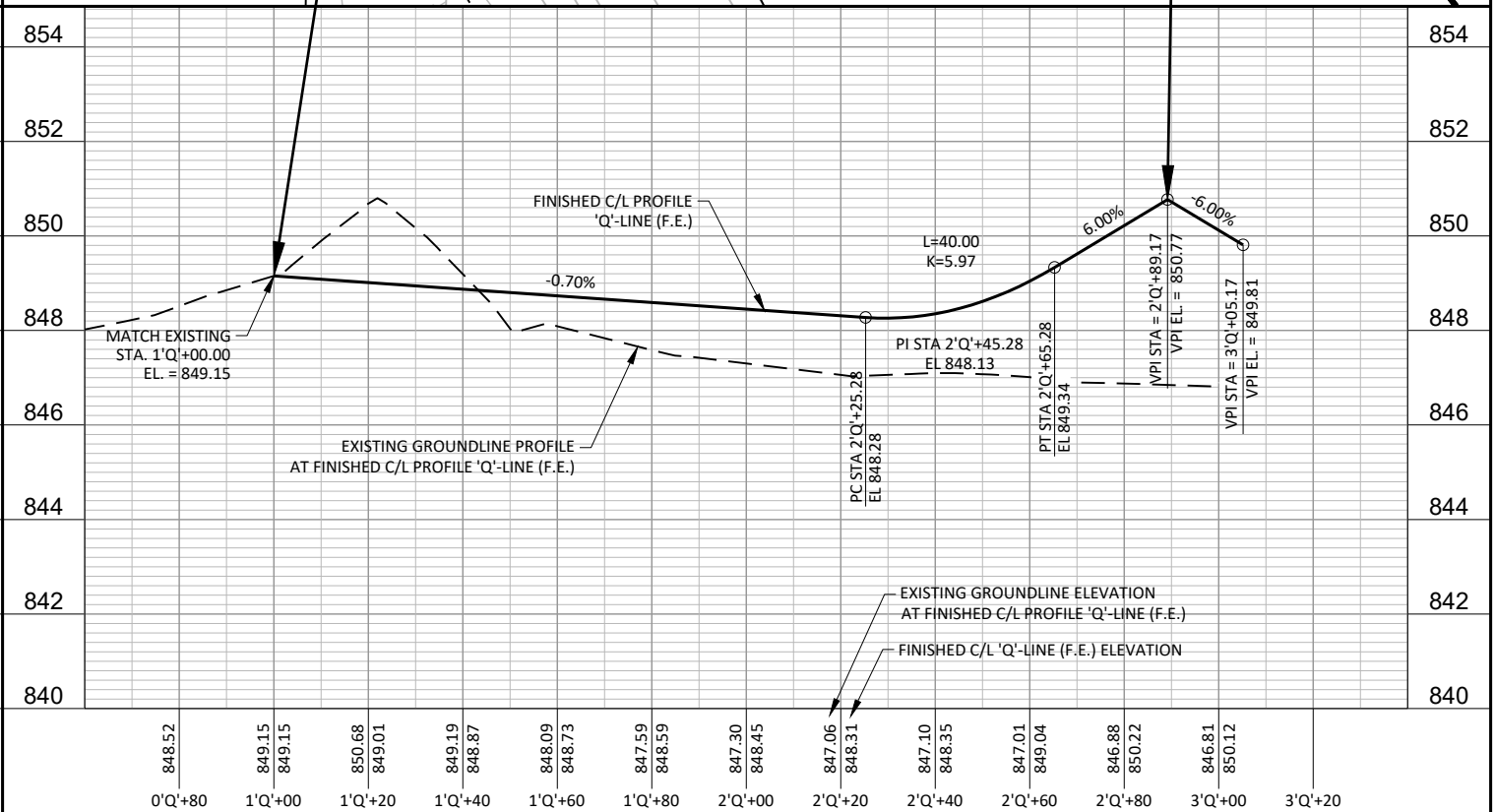
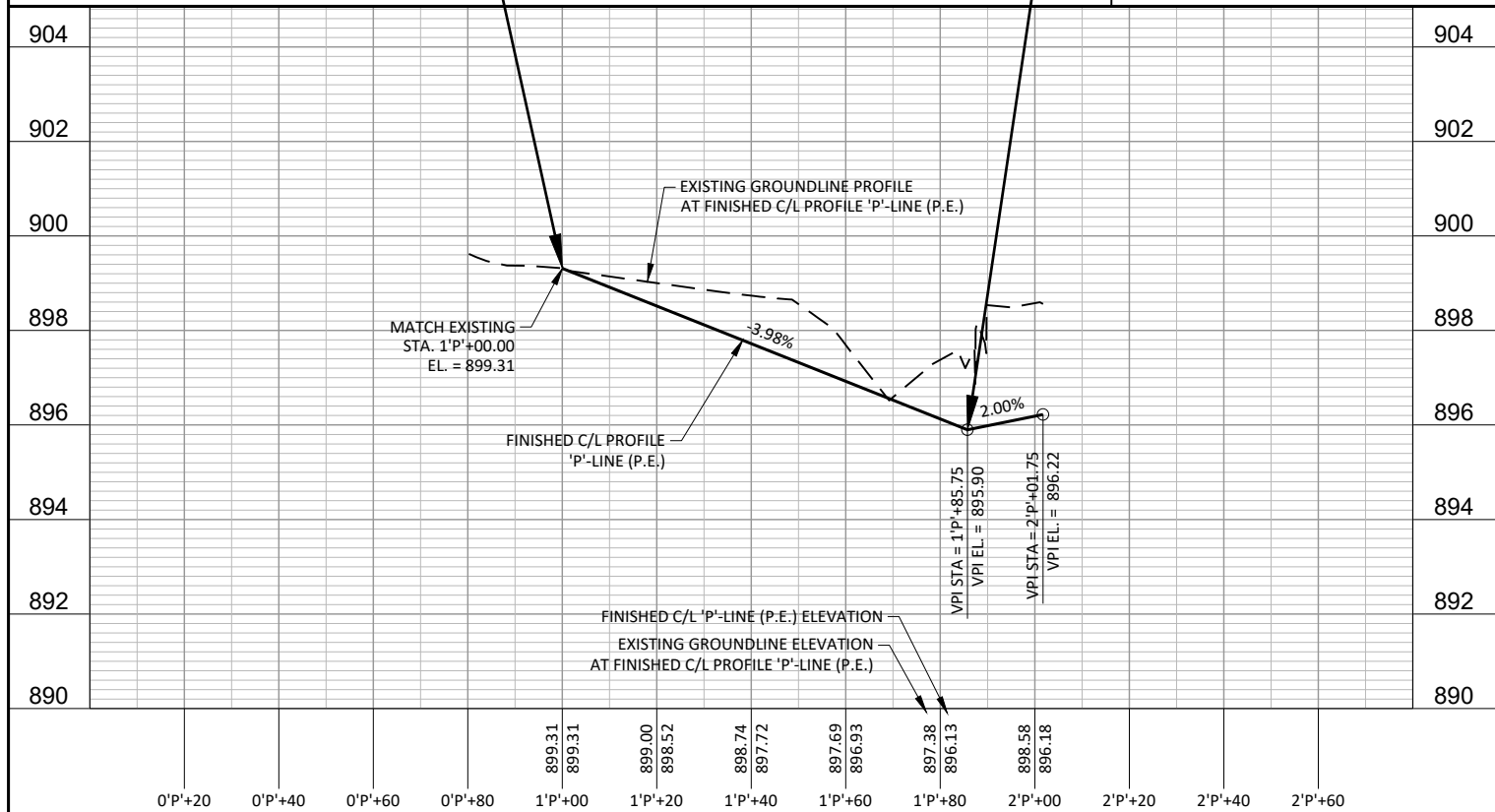
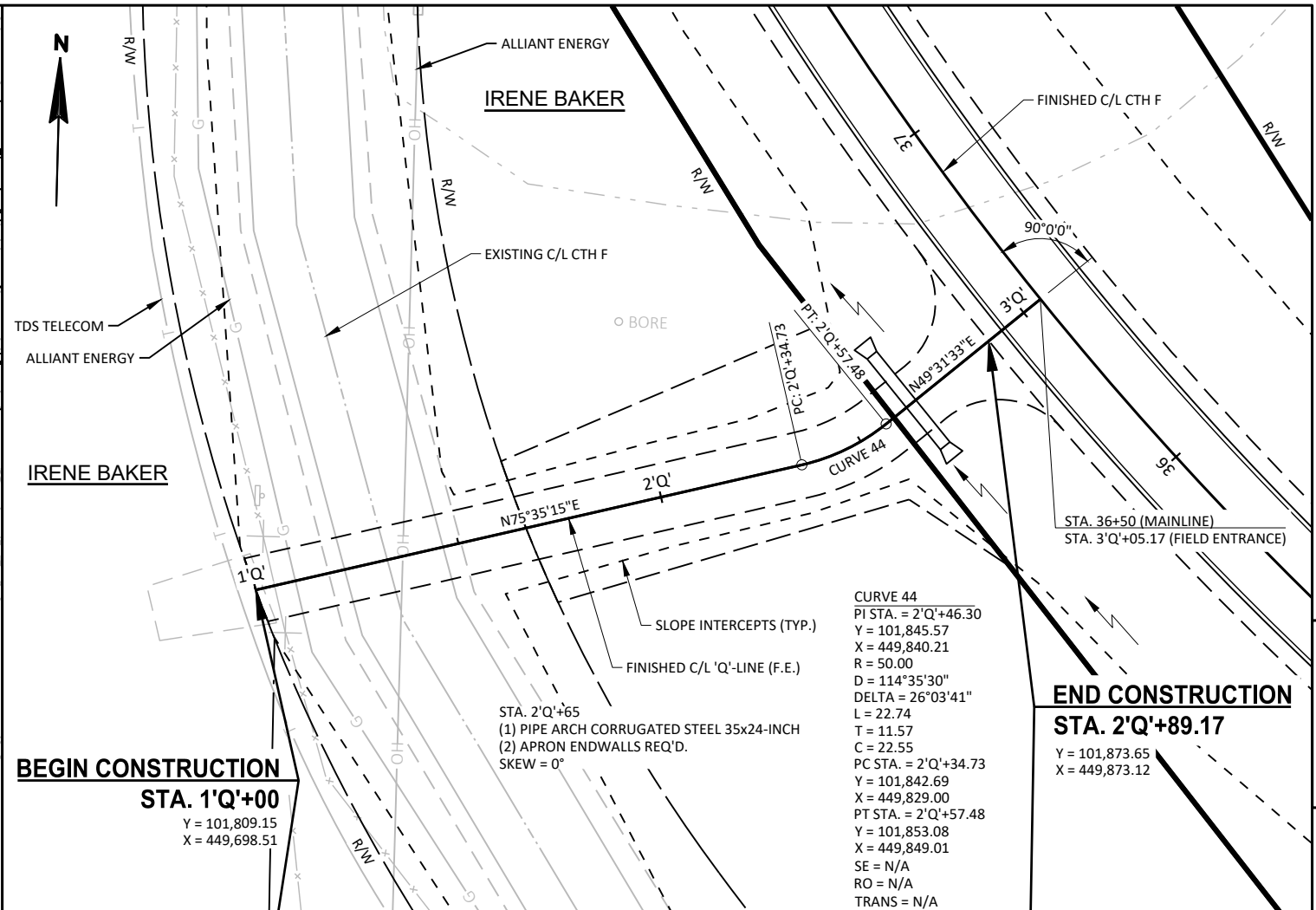
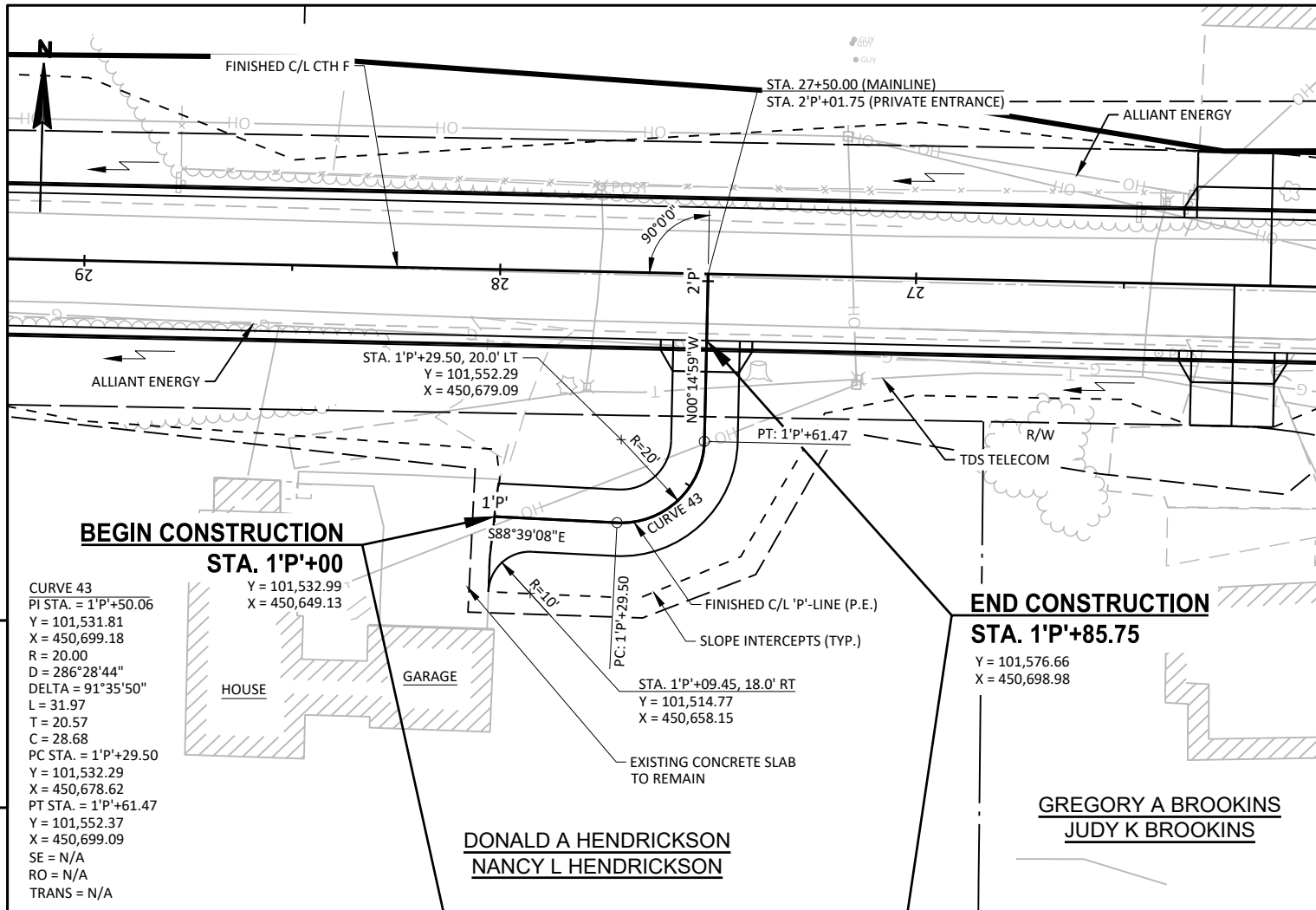
PROJECT NO: 5034-00-72 HWY: CTH F COUNTY: IOWA PLAN AND PROFILE: 'J'-LINE (STH 39) SHEET: E

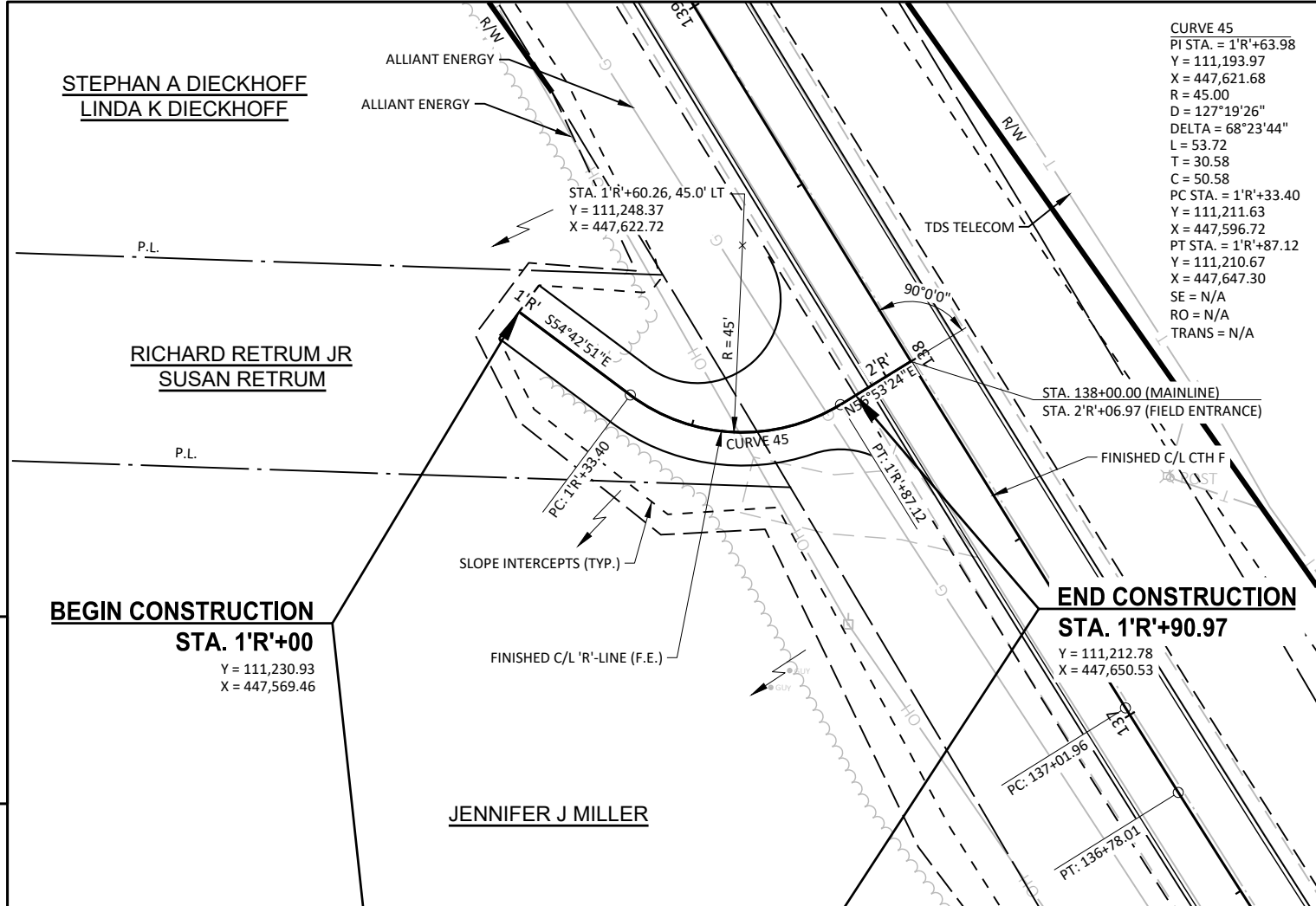




PROJECT NO: 5034-00-72 HWY: CTH F COUNTY: IOWA PLAN AND PROFILE: 'M'-LINE (F.E.) / 'O'-LINE (N. MAIN STREET) SHEET E



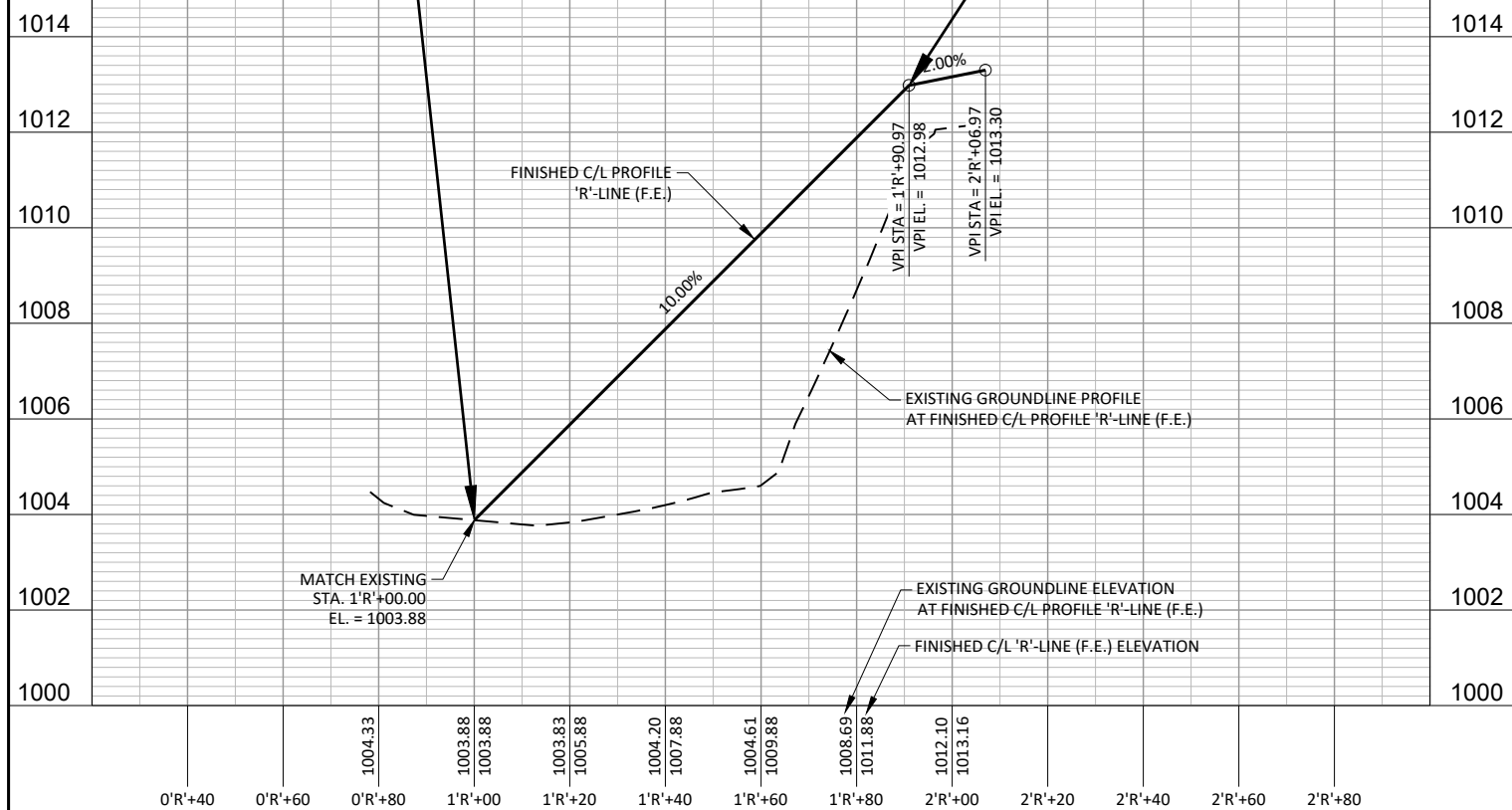




CURVE 45
 PI STA. = 1'R'+63.98
 Y = 111,193.97
 X = 447,621.68
 R = 45.00
 D = 127°19'26"
 DELTA = 68°23'44"
 L = 53.72
 T = 30.58
 C = 50.58
 PC STA. = 1'R'+33.40
 Y = 111,211.63
 X = 447,596.72
 PT STA. = 1'R'+87.12
 Y = 111,210.67
 X = 447,647.30
 SE = N/A
 RO = N/A
 TRANS = N/A

5

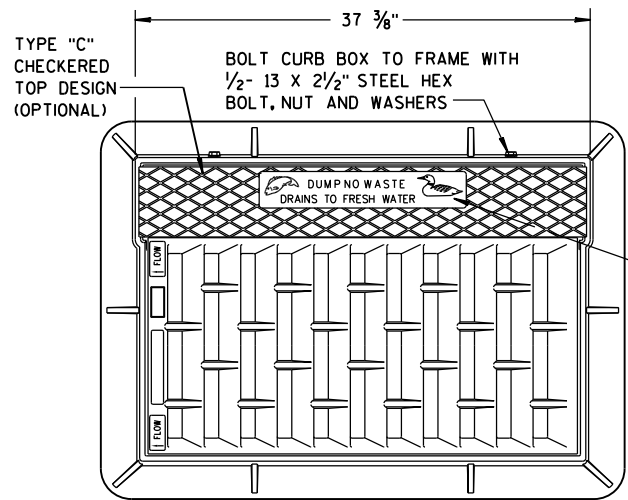
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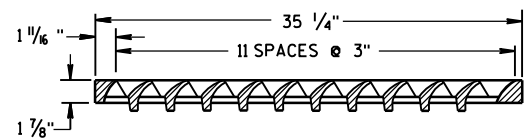
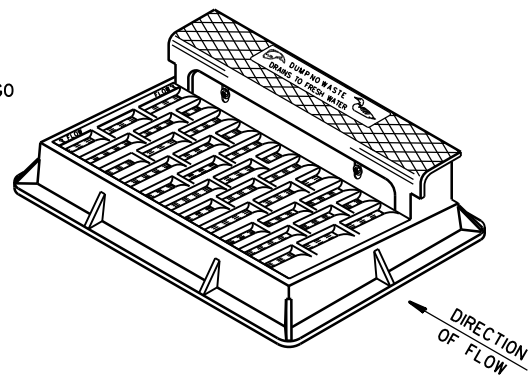
PROJECT NO: 5034-00-72 HWY: CTH F COUNTY: IOWA PLAN AND PROFILE: 'R'-LINE (F.E.) SHEET E

Standard Detail Drawing List

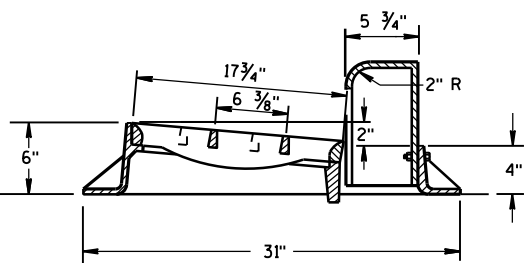
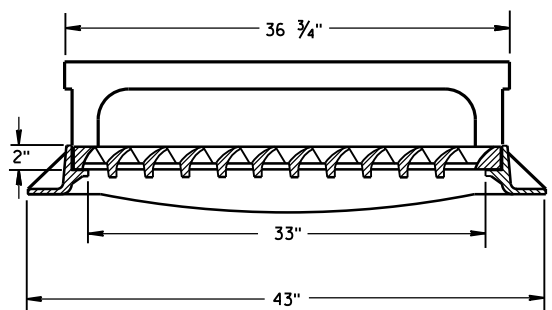
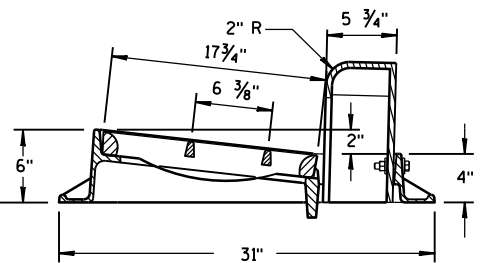
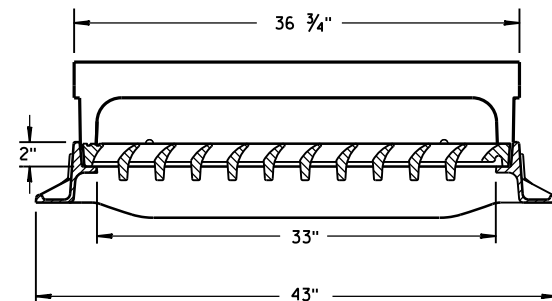
08A05-19A	INLET COVERS TYPE A, H, A-S, H-S & Z
08A05-19B	INLET COVERS TYPE B, B-A, C, MS, MS-A, & WM
08A05-19D	INLET COVER TYPE BW, MANHOLE COVERS, TYPE K, J, J-S, L & M
08B09-02	MANHOLES 3-FT, 4-FT, 5-FT, 6-FT, 7-FT AND 8-FT DIAMETER
08C07-02	INLETS 2X2-FT, 2X2.5-FT, 2X3-FT AND 2.5X3-FT
08C08-02	INLETS MEDIAN 1 AND 2 GRATE
08D01-22A	CONCRETE CURB & GUTTER
08D04-05	CONCRETE SURFACE DRAINS & ASPHALTIC FLUMES
08D20-01	DRIVEWAYS WITH CURB & GUTTER RETURNS
08D21-01	DRIVEWAYS WITHOUT CURB & GUTTER
08E08-03	TYPICAL INSTALLATIONS OF EROSION BALES / TEMPORARY DITCH CHECKS
08E09-06	SILT FENCE
08E10-02	INLET PROTECTION TYPE A, B, C AND D
08E15-01	CULVERT PIPE CHECK
08F01-11	APRON ENDWALLS FOR CULVERT PIPE
08F02-01	APRON ENDWALLS FOR PIPE ARCH AND ELLIPTICAL PIPE
08F03-03	DETAILS FOR PIPE CATTLE PASS, CONCRETE ENDWALLS AND STEPS
08F04-07	JOINT TIES FOR CONCRETE PIPE AND CONCRETE COLLAR DETAIL
09A01-13A	AT-GRADE SIDE ROAD INTERSECTION, TYPES "B1", "B2", "C" AND D AND TEE INTERSECTION BYPASS LANE
13A08-01	ASPHALTIC RUMBLE STRIPS AT INTERSECTION
13C19-03	HMA LONGITUDINAL JOINTS
15A03-02A	FLEXIBLE MARKER POST FOR CULVERT END
15A03-02B	FLEXIBLE MARKER POST FOR CULVERT END
15C02-08A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-08B	BARRICADES AND SIGNS FOR VARIOUS CLOSURES
15C03-05	BARRICADES AND SIGNS FOR SIDEROAD CLOSURES
15C04-05	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M. P. H. OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC
15C05-05	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M. P. H. OR LESS
15C08-20A	LONGITUDINAL MARKING (MAINLINE)
15C08-20C	PAVEMENT MARKING (TURN LANES)
15C12-07	TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION
15C19-06A	MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY
15C33-04	STOP LINE AND CROSSWALK PAVEMENT MARKING
15C35-04A	PAVEMENT MARKING (INTERSECTIONS)
15D28-04	TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY
16A01-07	LANDMARK REFERENCE MONUMENTS AND COVERS



**NOTE:
GRATE IS REVERSIBLE.**

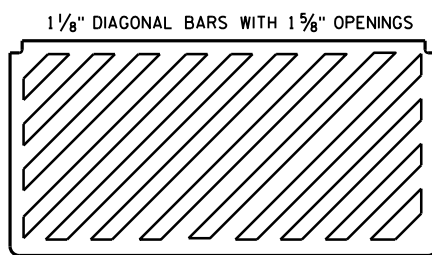


NOTE: CURB BOX HEIGHT ADJUSTABLE 6" TO 9"

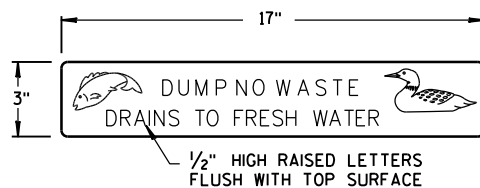


TYPE "H"

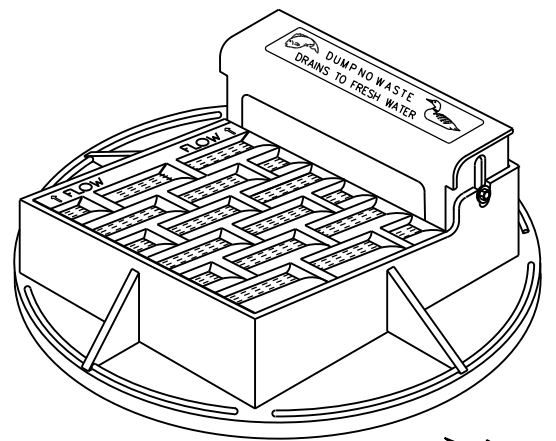
NOTE: EITHER CASTING IS ACCEPTABLE



**SPECIAL GRATE FOR
TYPE "H" COVER**
(MEASURES 35 1/4" X 17 3/4" X 2")
(NOTED AS TYPE H-S ON DRAINAGE TABLE)

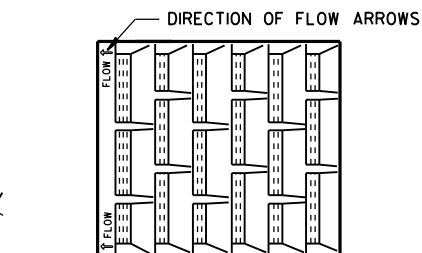
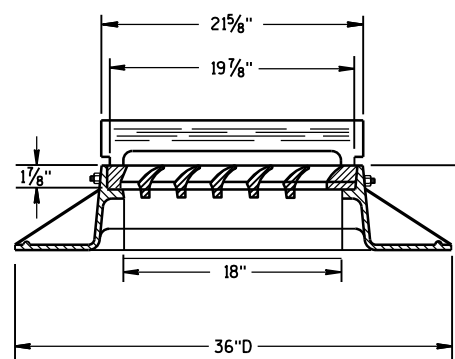


LOGO DETAIL

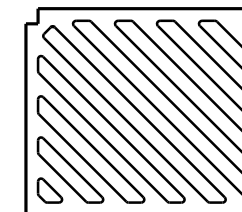


NOTE: CURB BOX ADJUSTABLE 4" TO 9"

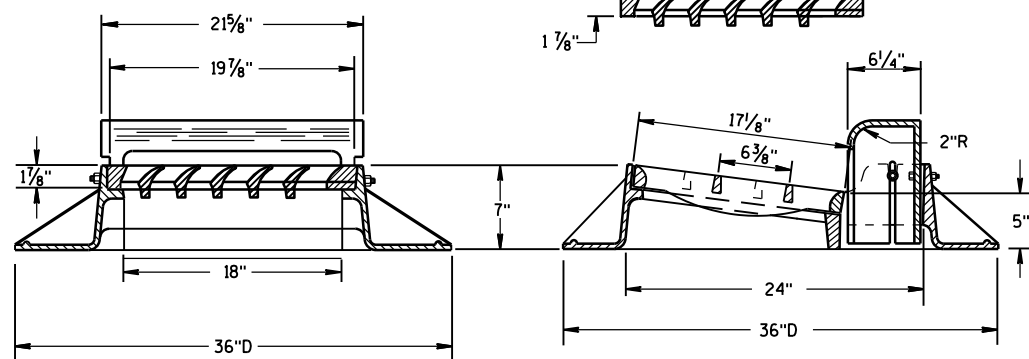
**NOTE:
GRATE IS REVERSIBLE.**



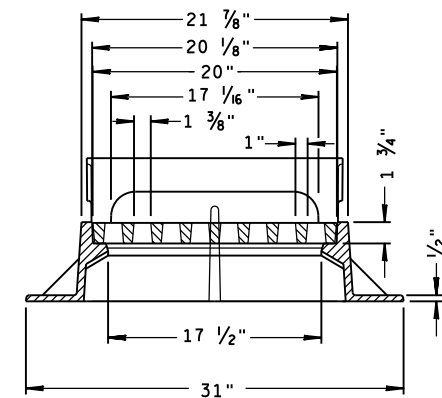
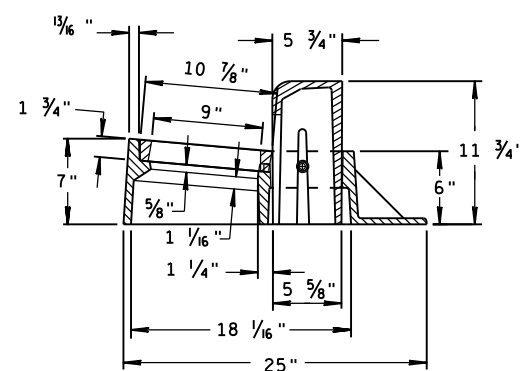
**1" DIAGONAL BARS
WITH 1 1/2" OPENINGS**



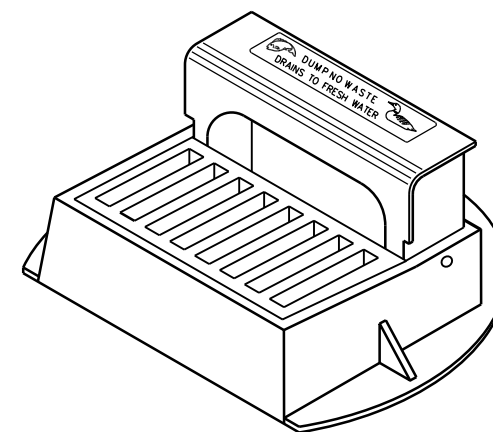
**SPECIAL GRATE FOR
TYPE "A" COVER**
(MEASURES 19 3/4" X 17" X 1 1/8")
(NOTED AS TYPE A-S ON DRAINAGE TABLE)



TYPE "A"



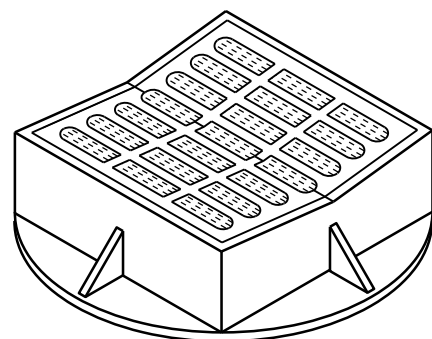
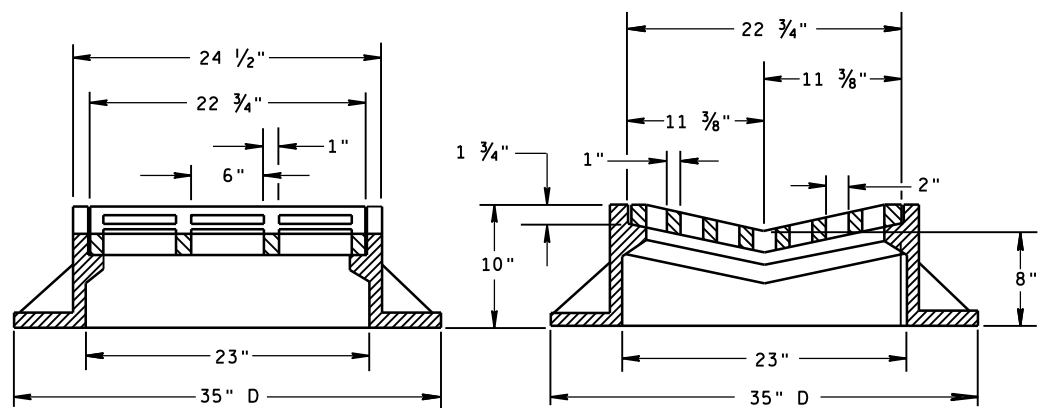
TYPE "Z"



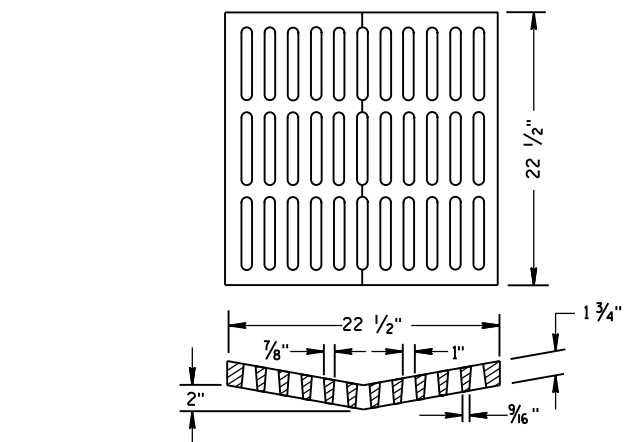
**INLET COVERS
TYPE A, H, A-S, H-S & Z**

**STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION**

APPROVED
11-27-13
DATE
/S/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT
ENGINEER
FHWA

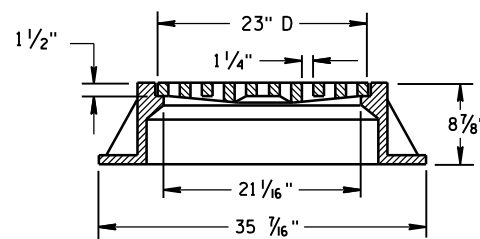
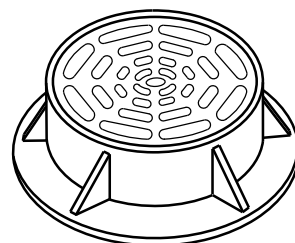
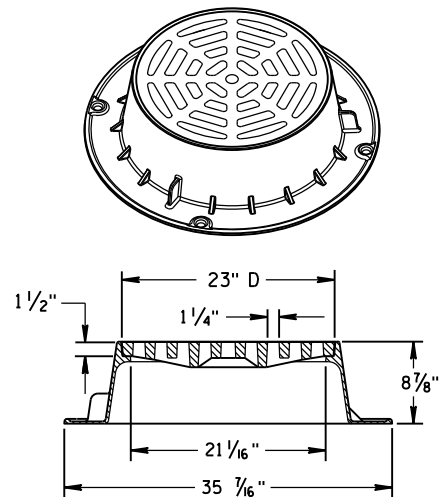


TYPE "B"



ALTERNATIVE GRATE FOR TYPE "B" COVER

USE WHERE PEDESTRIAN OR BICYCLE TRAFFIC IS POSSIBLE.
NOTED AS TYPE B-A ON THE DRAINAGE TABLE



TYPE "C"

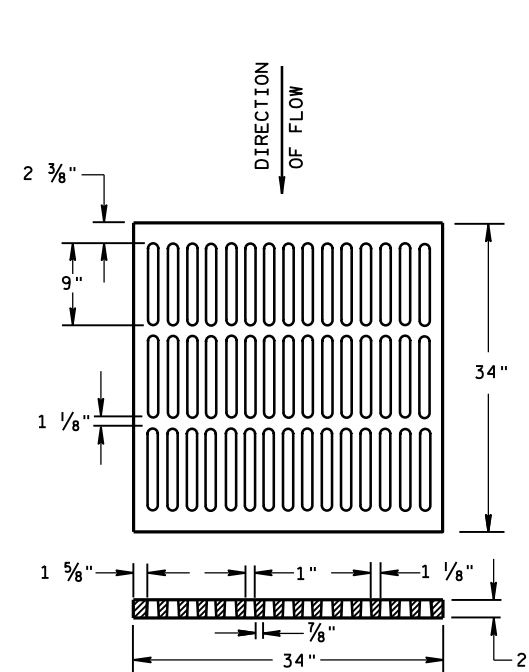
NOTE: EITHER CASTING IS ACCEPTABLE

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

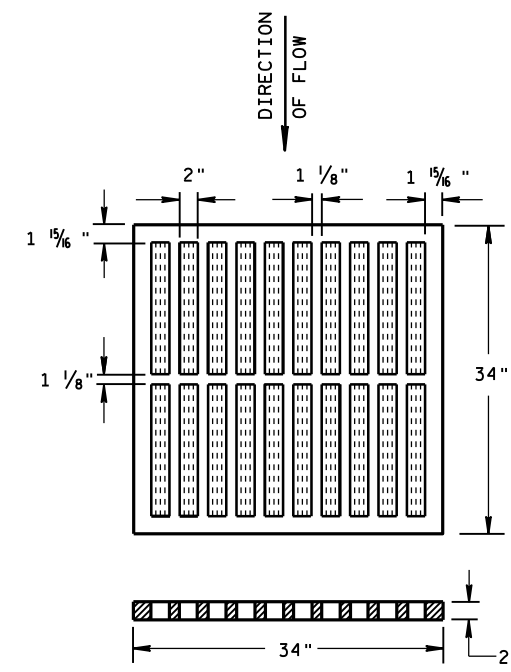
DETAIL DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR CATCH BASIN, MANHOLE AND INLET COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ROUND FRAMES AND COVERS SHALL HAVE CONTINUOUSLY MACHINED BEARING SURFACES TO PREVENT ROCKING AND RATTLING.



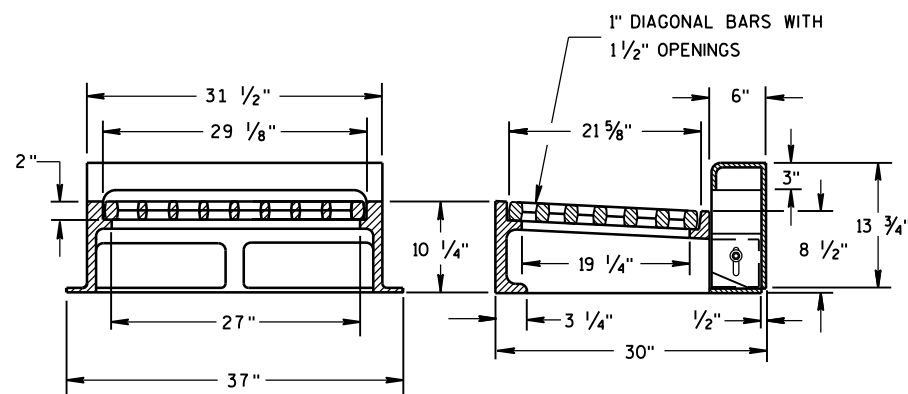
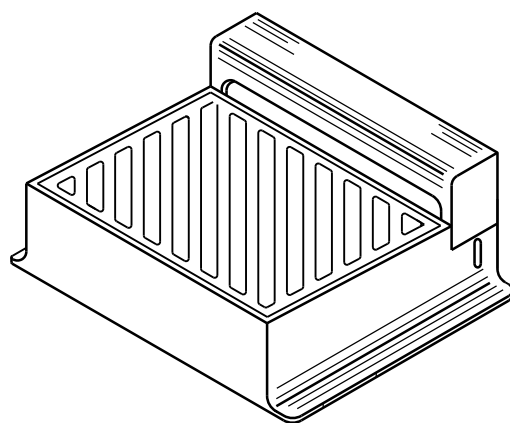
ALTERNATIVE TYPE "MS"

USE WHERE PEDESTRIAN OR BICYCLE TRAFFIC IS PERMITTED
NOTED AS TYPE MS-A ON THE DRAINAGE TABLE



TYPE "MS"

USE ON FREEWAYS AND EXPRESSWAYS
NOTED AS TYPE MS ON DRAINAGE TABLE



NOTE: CURB BOX HEIGHT ADJUSTABLE 6" TO 9"

TYPE "WM"

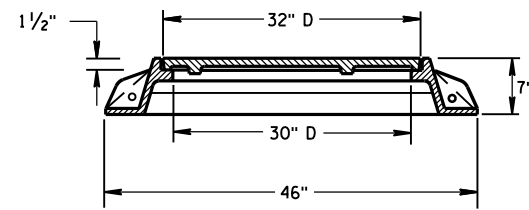
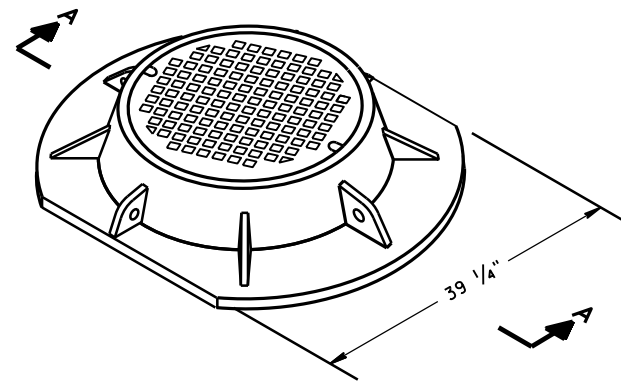
DIAGONAL SLOTS, SHALL BE ORIENTED TO THE DIRECTION OF FLOW AS ILLUSTRATED. GRATES ARE MANUFACTURED TO BE REVERSIBLE.

DIRECTION OF FLOW

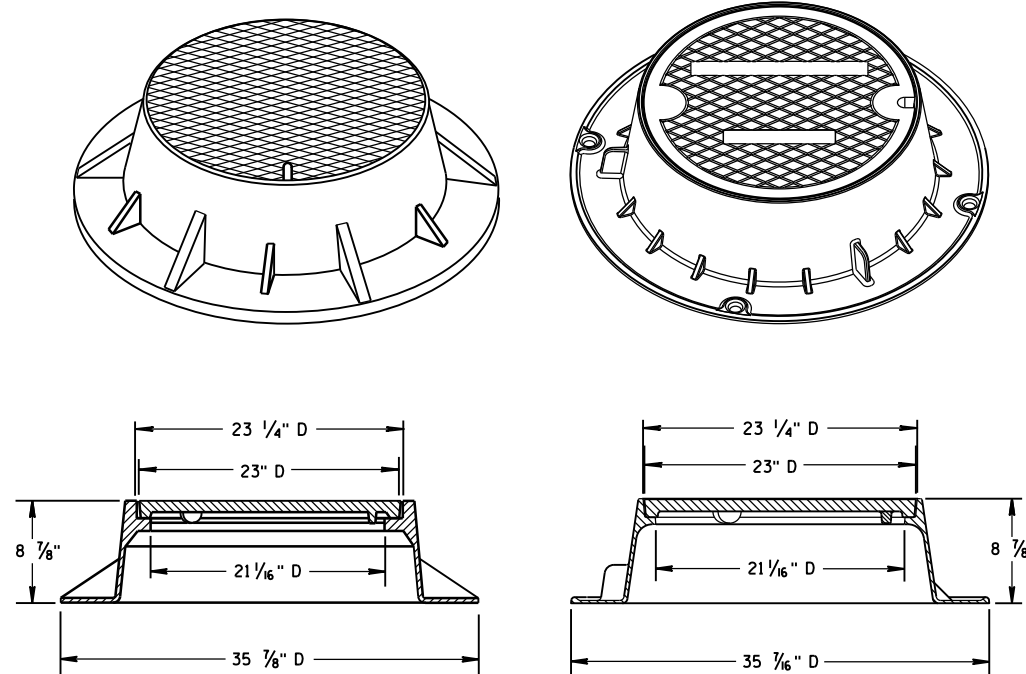
**INLET COVERS
TYPE B, B-A, C,
MS, MS-A, & WM**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE 11/27/2013 /S/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA



SECTION A-A
TYPE "K"



TYPE "J"

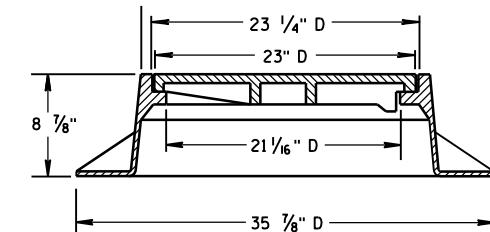
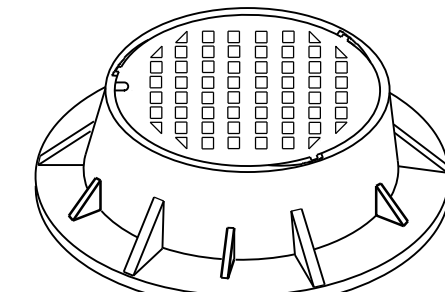
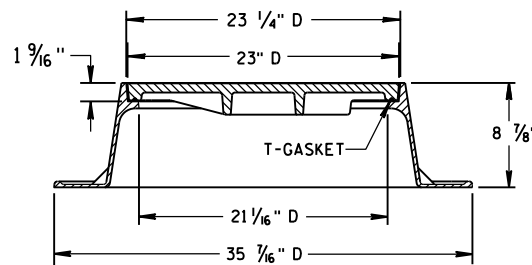
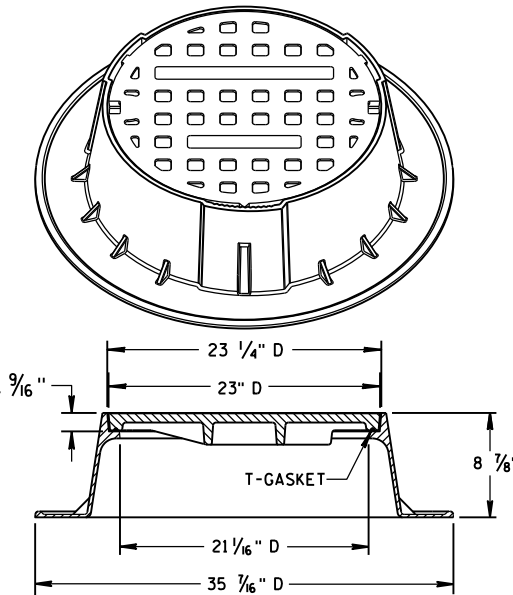
NOTE: EITHER CASTING IS ACCEPTABLE

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

DETAIL DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR MANHOLE COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ROUND FRAMES AND COVERS SHALL HAVE CONTINUOUSLY MACHINED BEARING SURFACES TO PREVENT ROCKING AND RATTLING.



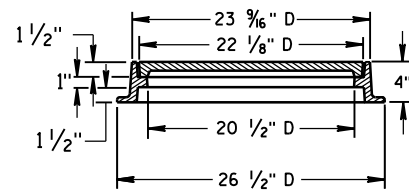
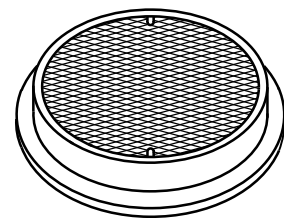
TYPE "J" SPECIAL

TYPE "B" NON-ROCKING SELF-SEAL LID

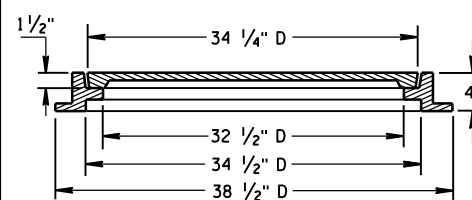
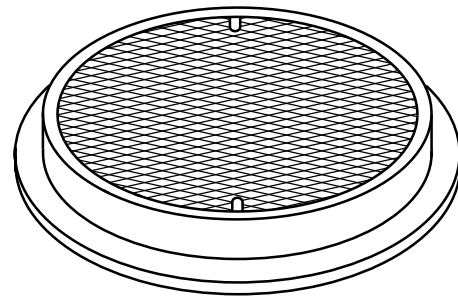
(NOTED AS TYPE J-S ON THE DRAINAGE TABLE)

NOTE: EITHER CASTING IS ACCEPTABLE

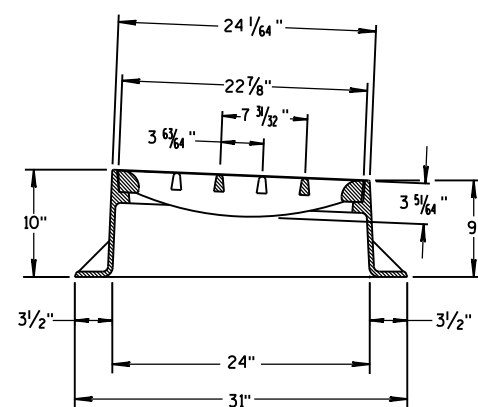
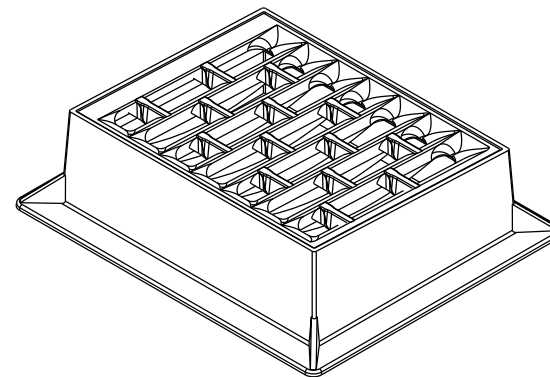
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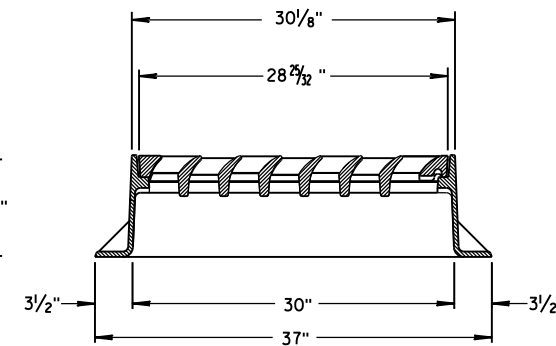
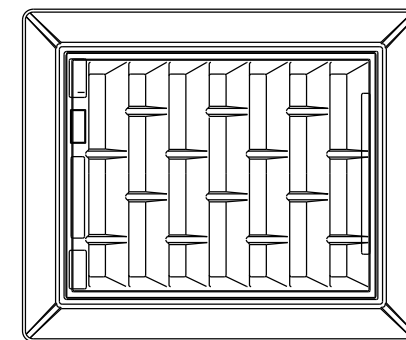
TYPE "L"



TYPE "M"



INLET COVER TYPE "BW"



6

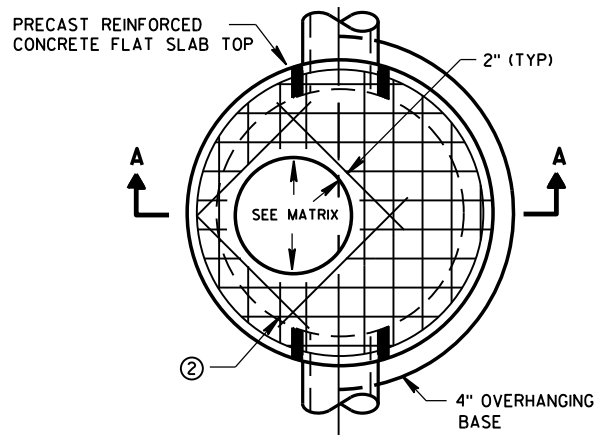
S.D.D. 8 A 5-19d

S.D.D. 8 A 5-19d

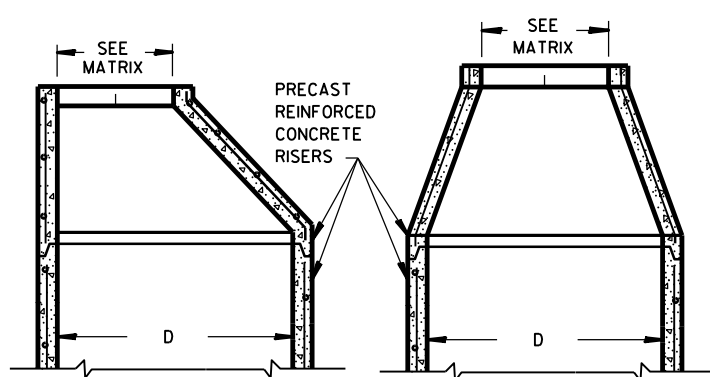
INLET COVER TYPE BW
MANHOLE COVERS, TYPE K,
J, J-S, L & M

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
11/27/2013 DATE /S/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT
ENGINEER
FHWA

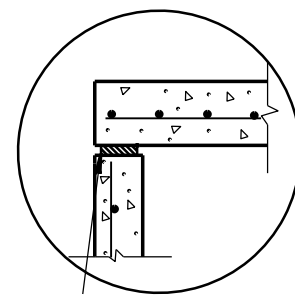


PLAN VIEW CIRCULAR OPENING

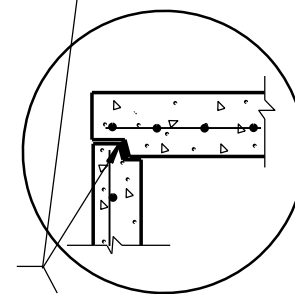


OPTIONAL PRECAST REINFORCED CONCRETE ECCENTRIC TOP

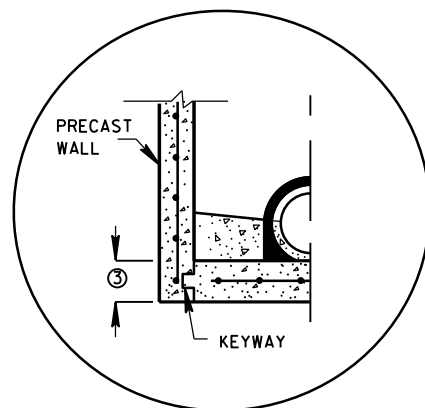
OPTIONAL PRECAST REINFORCED CONCRETE CONCENTRIC TOP



TOP WITH PLAIN END JOINT

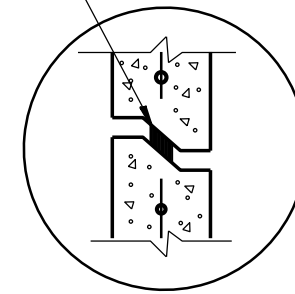


TOP WITH TONGUE AND GROOVE JOINT



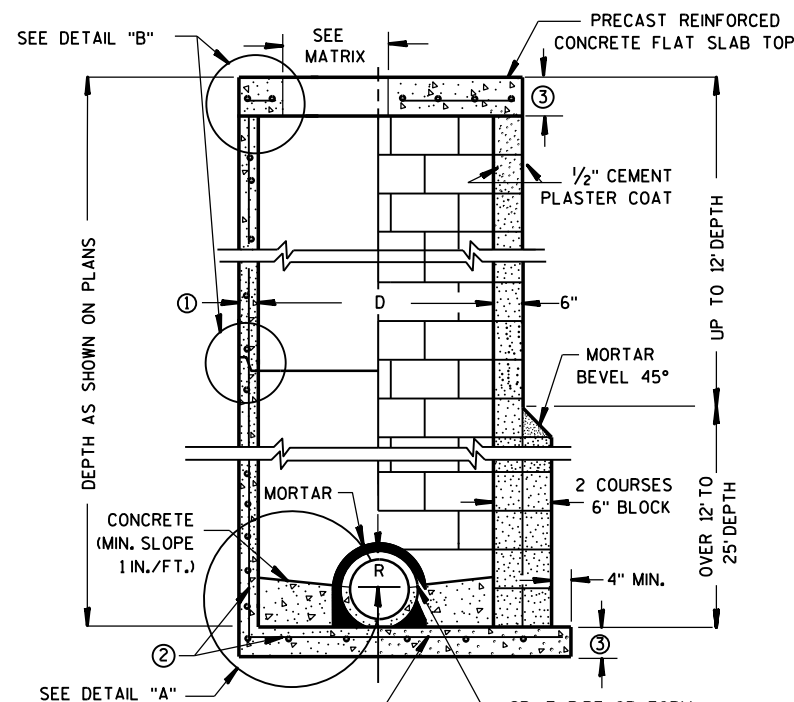
PRECAST REINFORCED CONCRETE WITH INTEGRAL BASE OPTION

JOINTS TO BE SEALED WITH A BUTYL RUBBER SEAL PER SEALANT MANUFACTURERS RECOMMENDATIONS CONFORMING TO ASTM C990 (TYP)



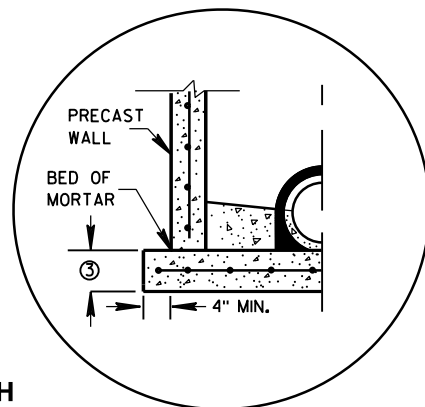
RISER WITH TONGUE AND GROOVE JOINT

DETAIL "B"



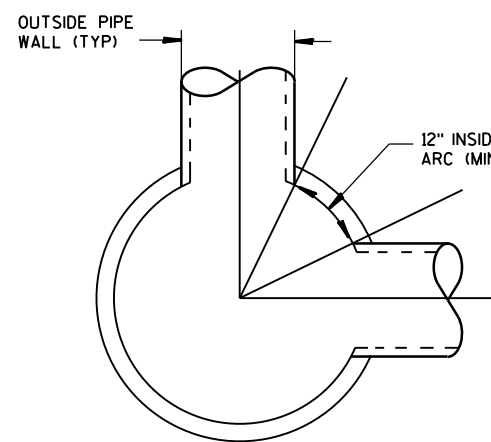
CONTRACTOR TO PROVIDE DRAWING(S) STAMPED BY A PROFESSIONAL ENGINEER FOR STEEL REINFORCING DESIGN FOR CAST-IN-PLACE STRUCTURES

PRECAST REINFORCED CONCRETE BLOCK WITH CONCRETE WITH MONOLITHIC BASE CAST-IN-PLACE OR PRECAST REINFORCED CONCRETE BASE ②



SEPARATE PRECAST REINFORCED CONCRETE BASE OPTION

DETAIL "A"



DETAIL "C"

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS. UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST MANHOLE UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL DRAINAGE STRUCTURES ARE DESIGNATED ON THE PLANS AS "MANHOLES 3X3-L", "CATCH BASINS 4-B", "INLETS 2X3-H", ETC. THE FIRST NUMBERS DESIGNATE THE SIZE OF THE STRUCTURE, AND THE FOLLOWING LETTER DESIGNATES THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF FOUNDATION BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

PRECAST REINFORCED CONE TOPS (ECCENTRIC OR CONCENTRIC) OR PRECAST REINFORCED FLAT SLAB TOPS MAY BE USED ON CONCRETE BLOCK STRUCTURES.

ECCENTRIC CONE TOPS MAY BE USED ON ALL STRUCTURES, AND CONCENTRIC CONE TOPS SHALL BE USED ONLY ON STRUCTURES 5 FEET OR LESS IN DEPTH, UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

STEPS MEETING AASHTO M199 AND THE FOLLOWING REQUIREMENTS SHALL BE INSTALLED IN ALL STRUCTURES OVER 5 FEET IN DEPTH; 16 INCH C-C MAXIMUM SPACING; PROJECT A MINIMUM CLEAR DISTANCE OF 4 INCHES FROM THE WALL AT THE POINT OF EMBEDMENT; MINIMUM LENGTH OF 10 INCHES; MINIMUM WALL EMBEDMENT OF 3 INCHES. FERROUS METAL STEPS NOT PAINTED OR TREATED TO RESIST CORROSION SHALL HAVE A MINIMUM CROSS SECTIONAL DIMENSION OF 1 INCH.

STEPS OF APPROVED POLYPROPYLENE PLASTIC COATED REINFORCEMENT BAR ARE ACCEPTABLE. REINFORCING BAR MUST BE A MINIMUM OF 1/2" AND MEET THE REQUIREMENTS OF ASTM A615.

CERTIFICATION SHALL BE PROVIDED THAT INSTALLED STEPS WHEN TESTED IN ACCORDANCE WITH SECTION 10 OF AASHTO T280 CAN WITHSTAND A VERTICAL LOAD OF 800 LBS. AND A HORIZONTAL LOAD OF 400 LBS.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED. CONCRETE BLOCK WILL NOT BE PERMITTED FOR STRUCTURES GREATER THAN 4 FEET IN DIAMETER.

PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

ALL PRECAST MANHOLE UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF AASHTO DESIGNATION M 199.

4" OVERHANGING BASES ARE REQUIRED FOR ALL CONCRETE BLOCK INSTALLATIONS. 4" OVERHANG IS REQUIRED WHEN SEPARATE PRECAST BASE IS PROVIDED. OVERHANG IS NOT REQUIRED ON PRECAST STRUCTURES WITH AN INTEGRAL OR MONOLITHIC BASE.

FOR ADDITIONAL CONFIGURATIONS, MAINTAIN A MINIMUM OF 12 INCHES AS MEASURED FROM THE INSIDE OF THE STRUCTURE WALL BETWEEN THE OUTSIDE PIPE WALLS OF ADJACENT PIPES. SEE DETAIL "C".

① MINIMUM WALL THICKNESS SHALL BE 4 INCHES FOR 3-FT, 5 INCHES FOR 4-FT, 6 INCHES FOR 5-FT, 7 INCHES FOR 6-FT, 8 INCHES FOR 7-FT AND 9 INCHES FOR 8-FT DIAMETER PRECAST MANHOLES.

② FOR PRECAST MANHOLES PROVIDE REINFORCING STEEL IN ACCORDANCE TO AASHTO M199.

③ PRECAST FLAT SLAB TOPS AND BASES WITH A DIAMETER OF 48" AND LESS SHALL HAVE A MINIMUM THICKNESS OF 6". PRECAST FLAT SLAB TOPS AND BASES WITH A DIAMETER LARGER THAN 48" SHALL HAVE A MINIMUM THICKNESS OF 8".

MANHOLE COVER OPENING MATRIX

MANHOLE COVER TYPE	C	ALL J'S	K	L	M
OPENING SIZE (FT)					
2 DIA.	X	X		X	
3 DIA.			X		X

PIPE MATRIX

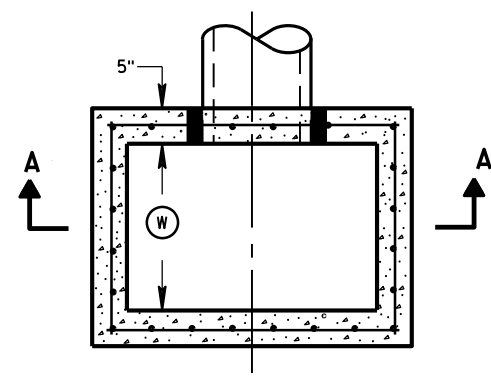
MANHOLE SIZE	MAXIMUM INSIDE PIPE DIAMETER FOR TWO PIPES	
	180° SEPARATION (IN)	90° SEPARATION (IN)
3-FT	15	12
4-FT	24	18
5-FT	36	24
6-FT	42	36
7-FT	48	36
8-FT	60	42

MANHOLES 3-FT, 4-FT, 5-FT, 6-FT, 7-FT AND 8-FT DIAMETER

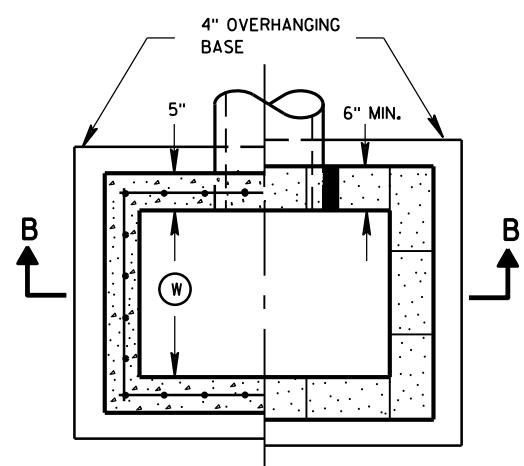
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED
 Sept., 2016 /S/ Rodney Taylor
 DATE ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR
 FHWA

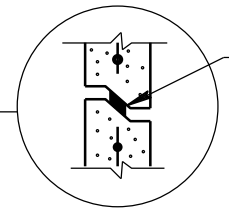
MANHOLES 3-FT, 4-FT, 5-FT, 6-FT, 7-FT AND 8-FT DIAMETER



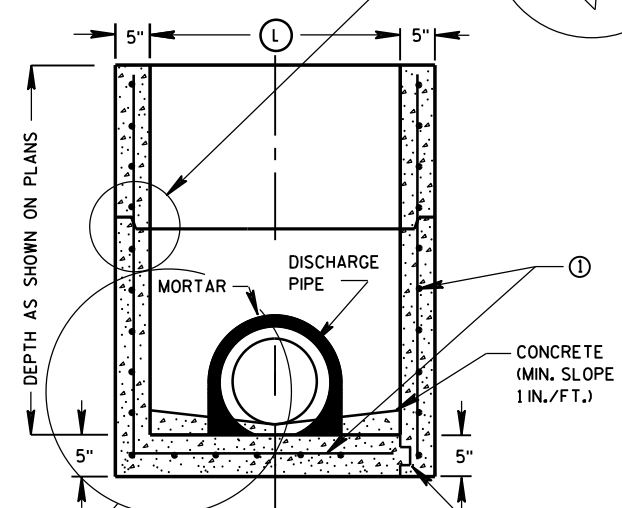
PLAN VIEW



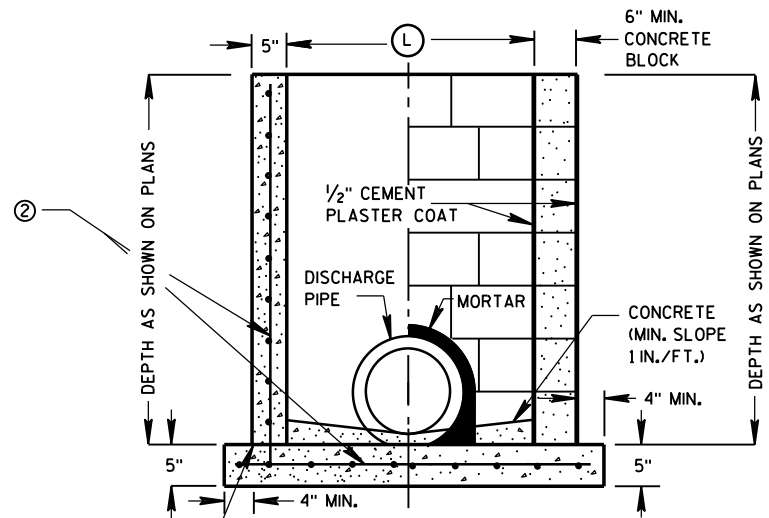
PLAN VIEW



RISER JOINTS TO BE SEALED WITH A BUTYL RUBBER SEAL PER SEALANT MANUFACTURERS RECOMMENDATIONS CONFORMING TO ASTM C 990 (TYP)



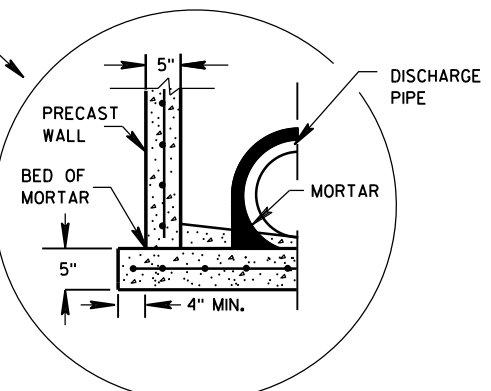
SECTION A-A



SECTION B-B

PRECAST REINFORCED CONCRETE WITH MONOLITHIC BASE
 PRECAST REINFORCED CONCRETE WITH INTEGRAL BASE
 KEYWAY

CONSTRUCTION JOINT
 CAST-IN-PLACE REINFORCED CONCRETE
 CONCRETE BLOCK WITH CAST-IN-PLACE OR PRECAST REINFORCED CONCRETE BASE ①



SEPARATE PRECAST REINFORCED CONCRETE BASE OPTION

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST INLET UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL PRECAST INLET UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF ASTM C 913.

ALL DRAINAGE STRUCTURES ARE DESIGNATED ON THE PLANS AS "MANHOLES 3X3-L", "CATCH BASINS 4-B", "INLETS 2X3-H", ETC. THE FIRST NUMBERS DESIGNATES THE SIZE OF THE STRUCTURE, AND THE FOLLOWING LETTER DESIGNATES THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF FOUNDATION BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

4" OVERHANGING BASES ARE REQUIRED FOR CAST-IN-PLACE REINFORCED CONCRETE AND CONCRETE BLOCK INSTALLATIONS. 4" OVERHANG IS REQUIRED WHEN SEPARATE PRECAST BASE IS PROVIDED. OVERHANG IS NOT REQUIRED ON PRECAST STRUCTURES WITH AN INTEGRAL OR MONOLITHIC BASE.

MAXIMUM INSIDE PIPE DIAMETER DETERMINED BY 3 INCH CLEARANCE ON EACH SIDE OF THE OUTSIDE WALL OF THE PIPE. SEE DETAIL "A". ASSUMES PIPE ENTERS PERPENDICULAR TO THE STRUCTURE.

① FOR PRECAST INLETS PROVIDE REINFORCING STEEL IN ACCORDANCE TO ASTM C 913.

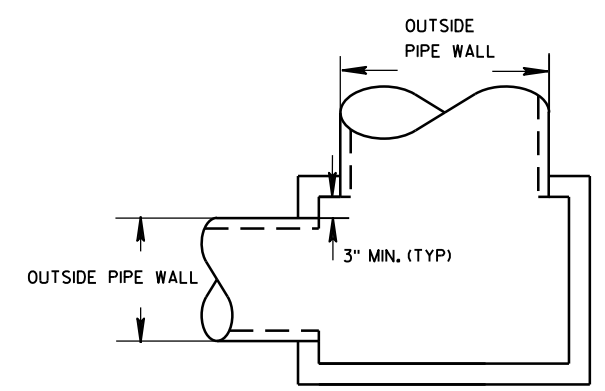
② CONTRACTOR TO PROVIDE DRAWING(S) STAMPED BY A PROFESSIONAL ENGINEER FOR STEEL REINFORCING DESIGN FOR CAST-IN-PLACE STRUCTURES.

INLET COVER MATRIX

INLET SIZE	INLET COVER TYPE		ALL A'S	ALL B'S	BW	F	ALL H'S	S	T	V	WM
	WIDTH ① (FT)	LENGTH ② (FT)									
2X2-FT	2	2	X	X				X		X	
2X2.5-FT	2	2.5			X			X	X	X	X
2X3-FT	2	3					X				
2.5X3-FT	2.5	3				X					

PIPE MATRIX

INLET SIZE	MAXIMUM INSIDE PIPE DIAMETER	
	WIDTH (IN)	LENGTH (IN)
2X2-FT	12	12
2X2.5-FT	12	18
2X3-FT	12	24
2.5X3-FT	18	24



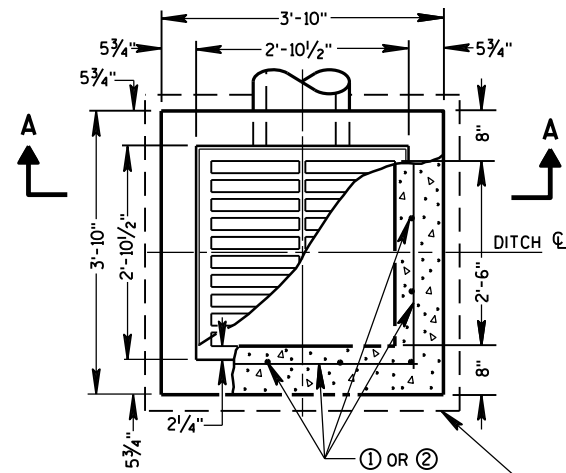
DETAIL "A"

INLETS 2X2-FT, 2X2.5-FT, 2X3-FT AND 2.5X3-FT

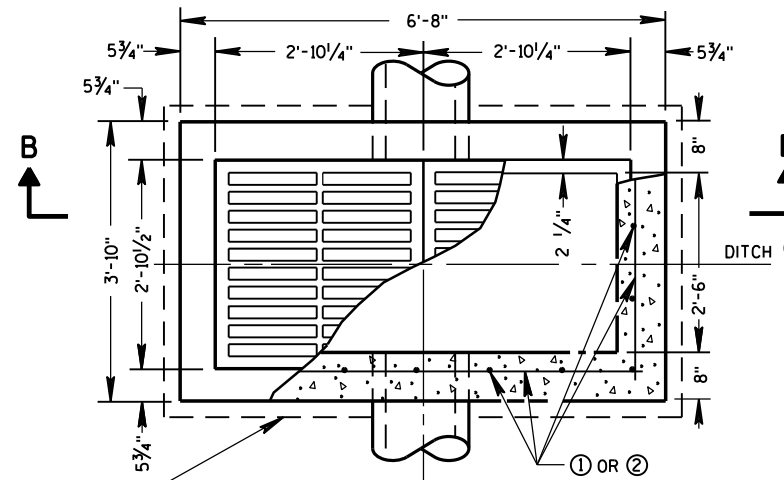
**INLETS 2X2-FT, 2X2.5-FT,
2X3-FT AND 2.5X3-FT**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
 Sept., 2016 /S/ Rodney Taylor
 DATE ROADWAY STANDARDS DEVELOPMENT
 FHWA UNIT SUPERVISOR

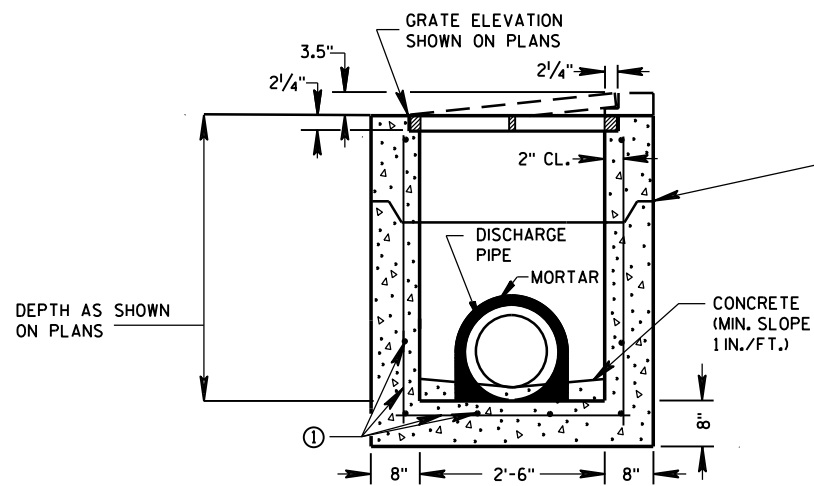


PLAN VIEW

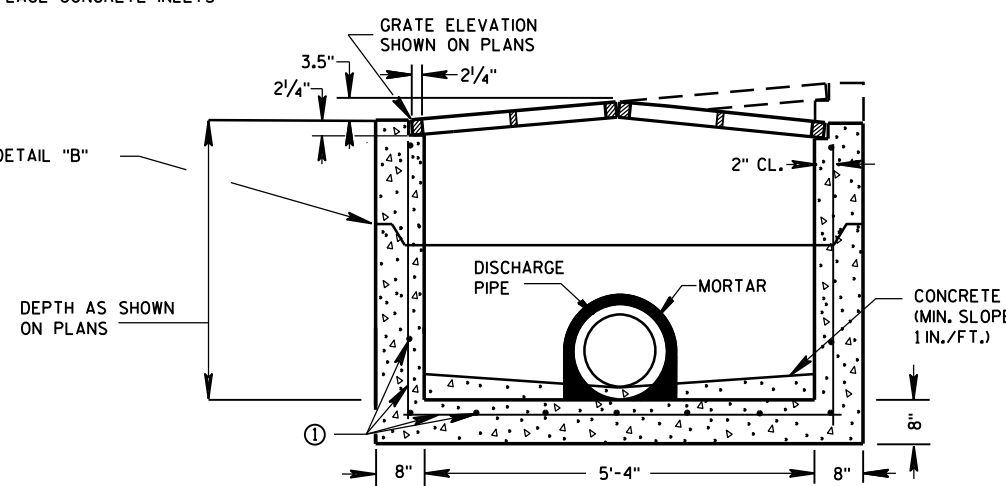


PLAN VIEW

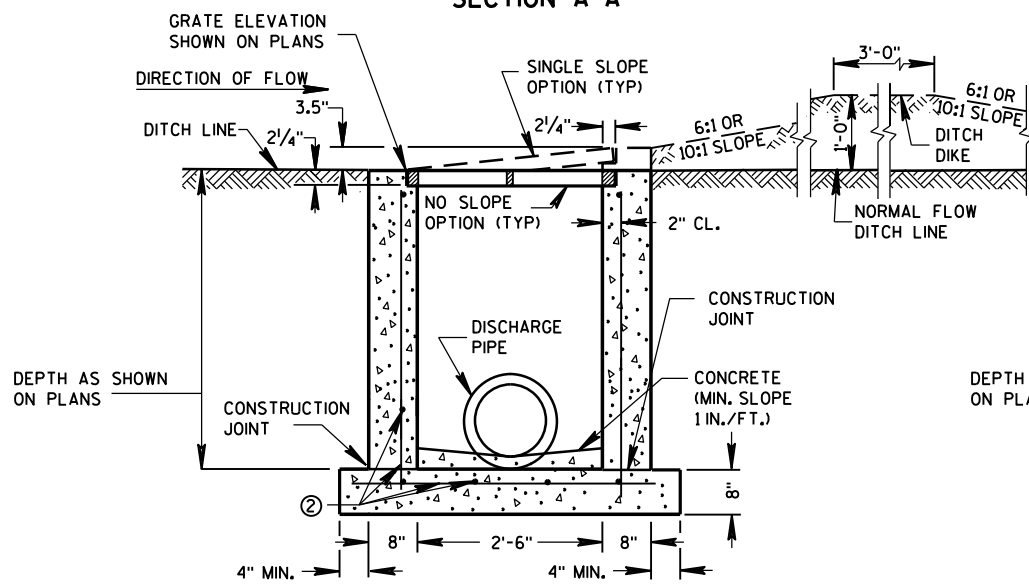
4" OVERHANGING BASE ON REINFORCED CAST-IN-PLACE CONCRETE INLETS



PRECAST REINFORCED CONCRETE SECTION A-A

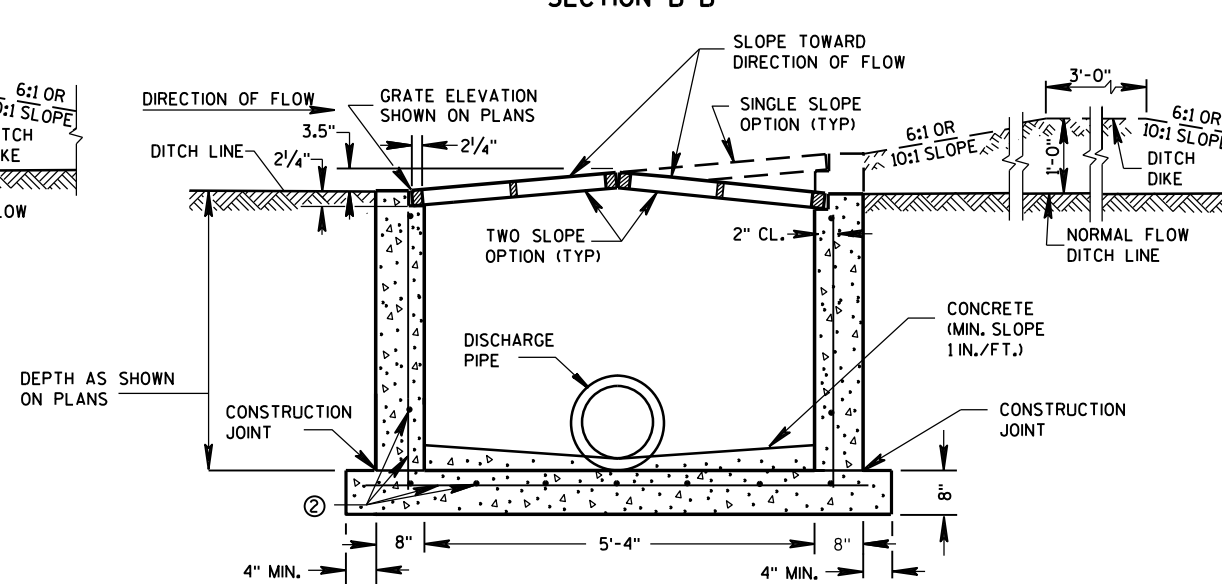


PRECAST REINFORCED CONCRETE SECTION B-B



REINFORCED CAST-IN-PLACE CONCRETE SECTION A-A

INLETS MEDIAN 1 GRATE



REINFORCED CAST-IN-PLACE CONCRETE SECTION B-B

INLETS MEDIAN 2 GRATE

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST INLET UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR INLETS WHICH MAY INCLUDE PRECAST REINFORCED CONCRETE INLETS, SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL MEDIAN INLETS ARE DESIGNATED ON THE PLANS AS "INLETS, IG-MS", ETC. THE FIRST NUMBER AND LETTER DESIGNATE THE TYPE OF STRUCTURE, AND THE FOLLOWING LETTERS DESIGNATE THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT. BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF FOUNDATION BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

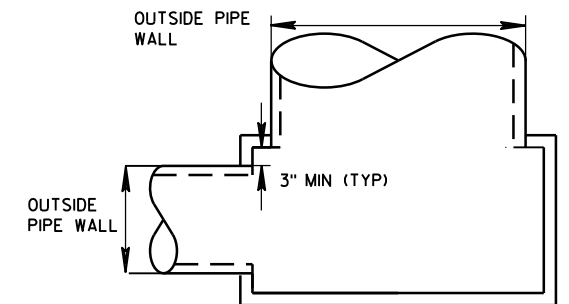
ALL PRECAST INLET UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF ASTM C 913.

MAXIMUM INSIDE PIPE DIAMETER DETERMINED BY 3" CLEARANCE ON EACH SIDE OF THE OUTSIDE WALL OF THE PIPE. SEE DETAIL "A". ASSUMES PIPE ENTERS PERPENDICULAR TO THE STRUCTURE.

- ① FOR PRECAST INLETS PROVIDE REINFORCING STEEL IN ACCORDANCE TO ASTM C 913.
- ② CONTRACTOR TO PROVIDE DRAWING(S) STAMPED BY A PROFESSIONAL ENGINEER FOR STEEL REINFORCING DESIGN FOR CAST-IN-PLACE STRUCTURES.

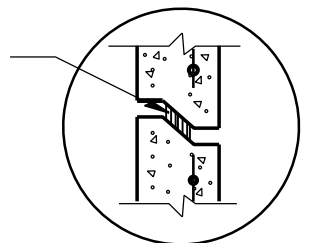
PIPE MATRIX

INLET SIZE	MAXIMUM INSIDE PIPE DIAMETER	
	WIDTH (IN)	LENGTH (IN)
1 GRATE	18	18
2 GRATE	18	42



DETAIL "A"

JOINTS TO BE SEALED WITH A BUTYL RUBBER SEAL PER SEALANT MANUFACTURERS RECOMMENDATIONS CONFORMING TO ASTM C 990 (TYP)

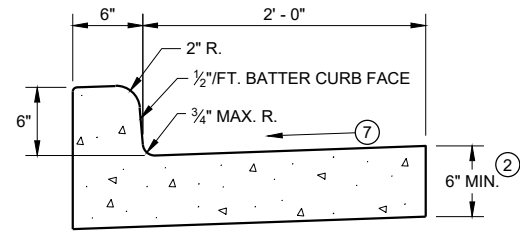


DETAIL "B"

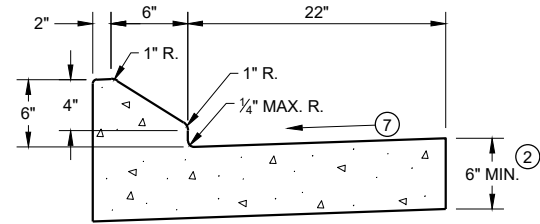
INLETS MEDIAN 1 AND 2 GRATE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

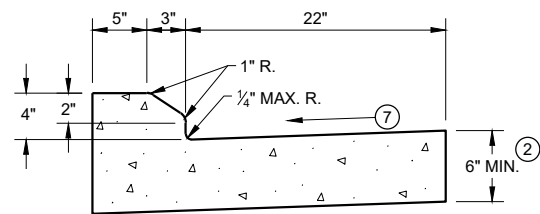
APPROVED
Sept., 2016 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
FHWA UNIT SUPERVISOR



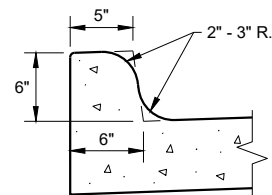
TYPES A¹ & D



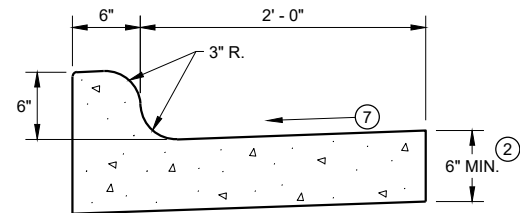
6" SLOPED CURB TYPES G¹ & J



4" SLOPED CURB TYPES G¹ & J

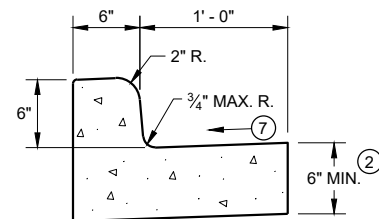


TYPES K¹ & L
(OPTIONAL CURB SHAPE)



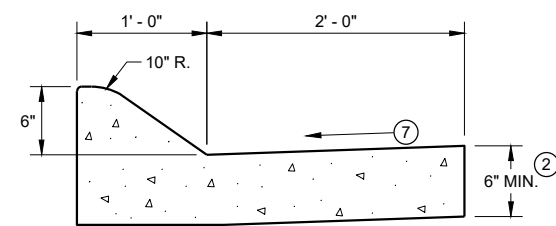
TYPES K¹ & L

CONCRETE CURB AND GUTTER 30"

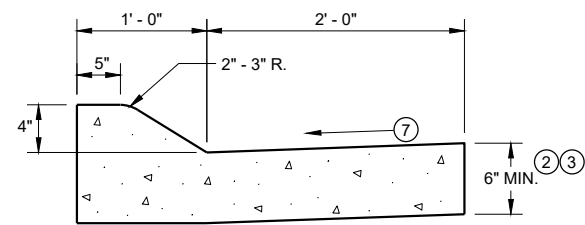


TYPES A¹ & D

CONCRETE CURB AND GUTTER 18"

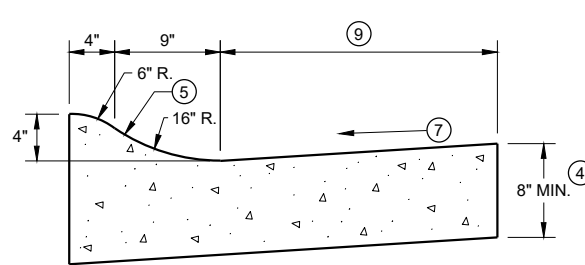


6" SLOPED CURB TYPES A¹ & D



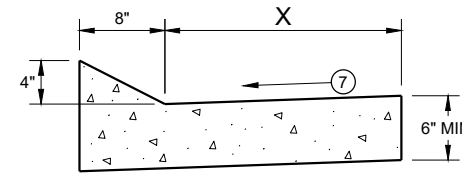
4" SLOPED CURB TYPES A¹ & D

CONCRETE CURB AND GUTTER 36"



4" SLOPED CURB TYPES R¹ & T

TBT & TBTT	X
30"	22"
36"	28"

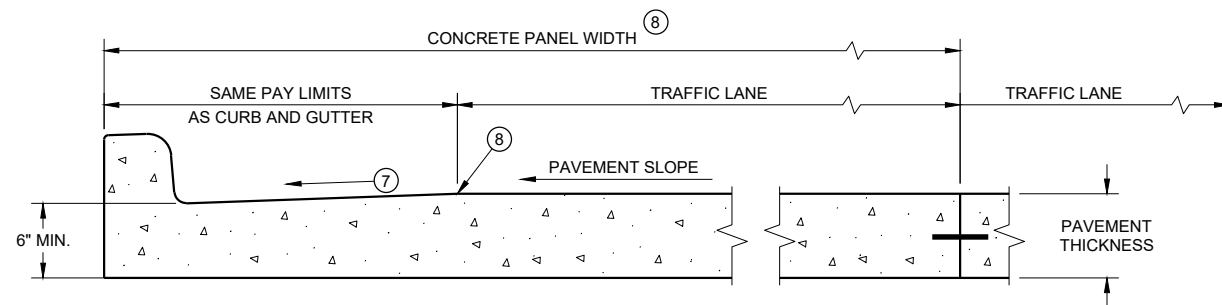


TYPES TBT & TBTT¹

CONCRETE CURB AND GUTTER

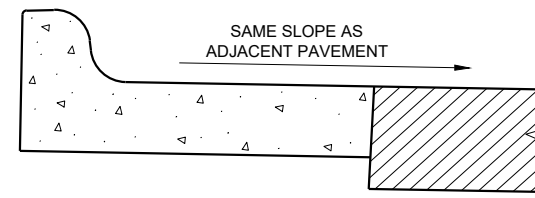
PAVEMENT THICKNESS AND MAXIMUM CONCRETE PANEL WIDTH TABLE

PAVEMENT THICKNESS	MAXIMUM PANEL WIDTH
LESS THAN 10"	12'
10" & ABOVE	15'



PARTIAL SECTION OF PAVEMENT *
WITH INTEGRAL CURB AND GUTTER

* BIKE LANE IS NOT SHOWN



REVERSE SLOPE GUTTER⁶
(TYPICAL FOR ALL CURB & GUTTER TYPES)

GENERAL NOTES

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

INTEGRAL CURB AND GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB AND GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE.

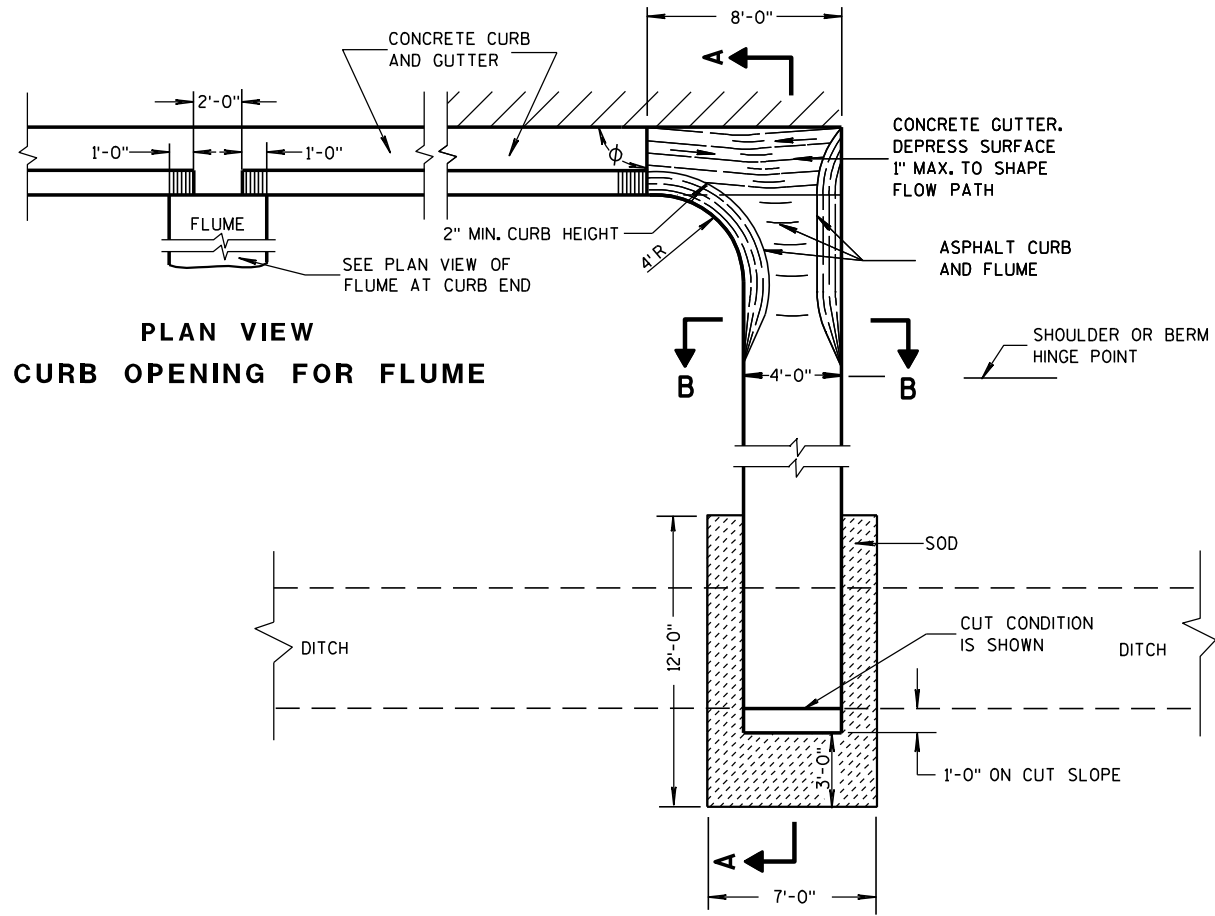
UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ③ USE 8" MINIMUM GUTTER THICKNESS WHEN USED WITH AN ADJACENT CONCRETE TRUCK APRON PLACED BEHIND BACK OF CURB.
- ④ THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑤ UNLESS OTHERWISE NOTED, FOR STAKING PURPOSES THE FACE OF CURB IS 6" FROM THE BACK OF CURB.
- ⑥ WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.
- ⑦ USE 4% GUTTER CROSS SLOPE UNLESS OTHERWISE NOTED IN THE PLANS.
- ⑧ INCLUDE LONGITUDINAL JOINT AND TIE BARS ALONG LANE EDGE WHEN CONCRETE PANEL WIDTH EXCEEDS THE MAXIMUM WIDTH PER TABLE BELOW. LONGITUDINAL JOINT(S) ARE NOT ALLOWED WITHIN TRAFFIC LANES AND BIKE LANES. LONGITUDINAL JOINT MAY BE SAWED.
- ⑨ CONCRETE CURB AND GUTTER 4-INCH SLOPED 30-INCH TYPE "R" AND "T" = 17 INCHES
CONCRETE CURB AND GUTTER 4-INCH SLOPED 36-INCH TYPE "R" AND "T" = 23 INCHES

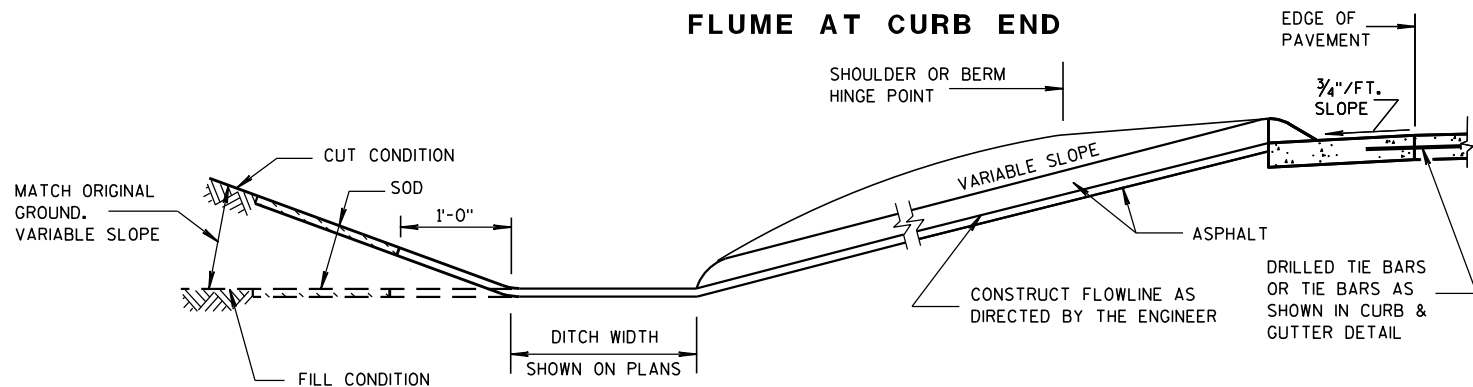
ASPHALTIC FLUME

NOTE: TAPER CURB ENDS TO GUTTER IN 1'-0"

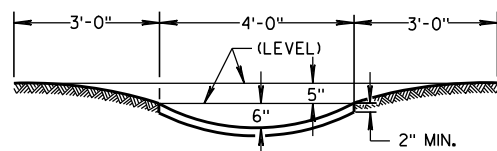
INCREASE ϕ FROM RIGHT ANGLE TO BEST FIT FIELD CONDITIONS



PLAN VIEW FLUME AT CURB END



SECTION A-A

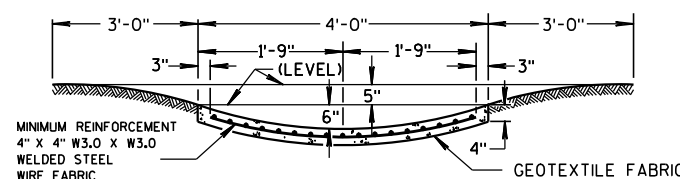
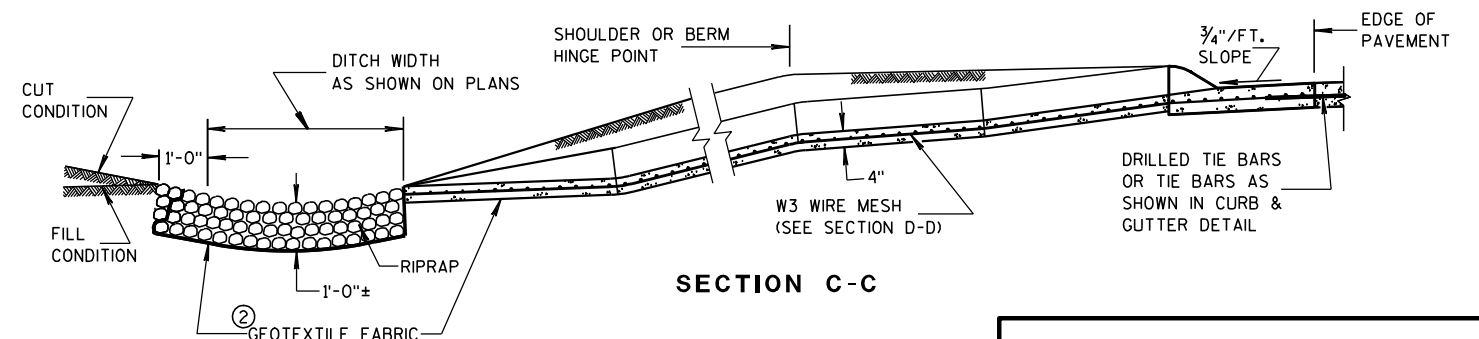
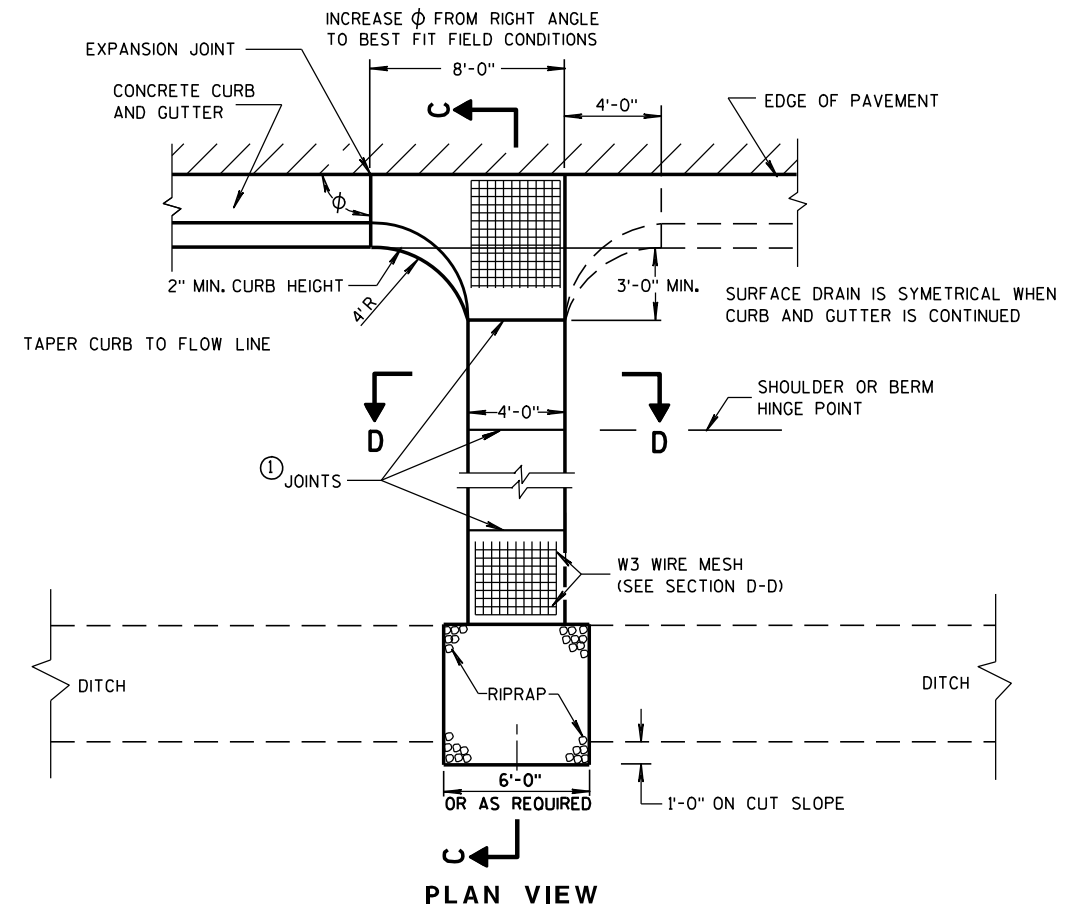


SECTION B-B

GENERAL NOTES

- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.
- WELDED STEEL WIRE FABRIC SHALL BE IN ACCORDANCE WITH AASHTO SPECIFICATION M55.
- JOINTS SHALL BE 1/8 TO 1/4 INCH WIDE BY 1 1/2 INCHES DEEP AND SPACED AT UNIFORM INTERVALS OF APPROXIMATELY 4 FEET.
 - GEOTEXTILE FABRIC TYPE "R" SHALL UNDERLAY THE FULL LENGTH AND WIDTH OF THE CONCRETE SURFACE DRAIN AND RIPRAP.
 - CONCRETE SURFACE DRAIN WITHOUT CURB AND GUTTER MAY BE USED ON BACKSLOPES WHEN SPECIFIED

CONCRETE SURFACE DRAIN

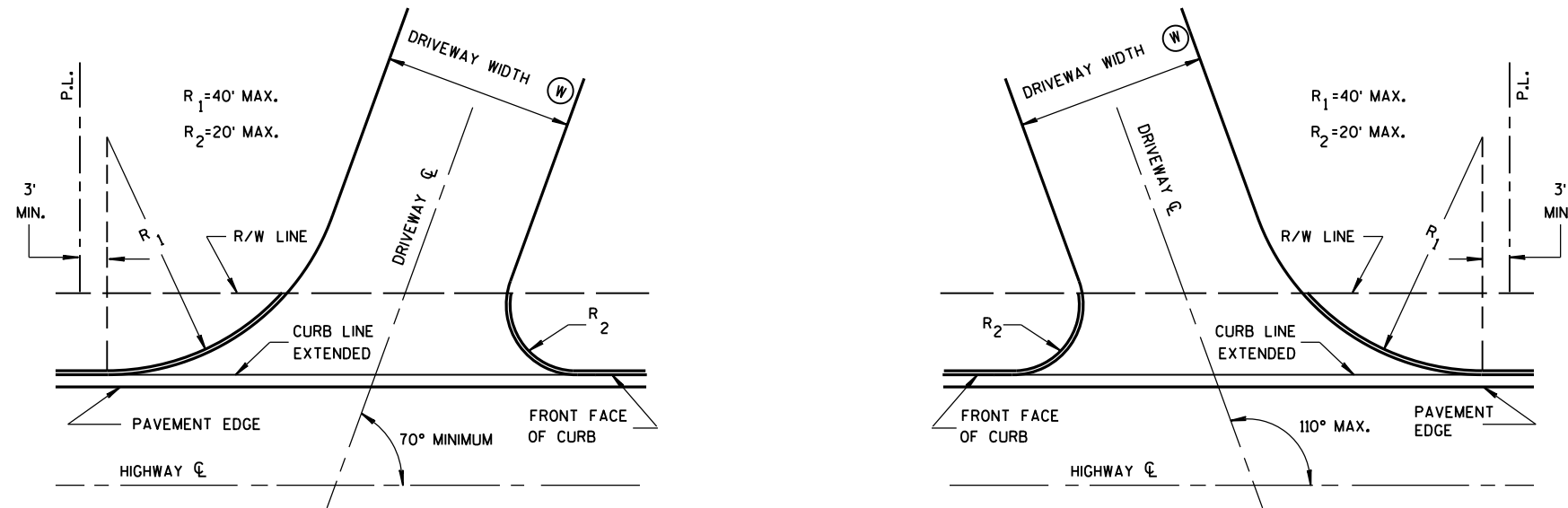


SECTION D-D

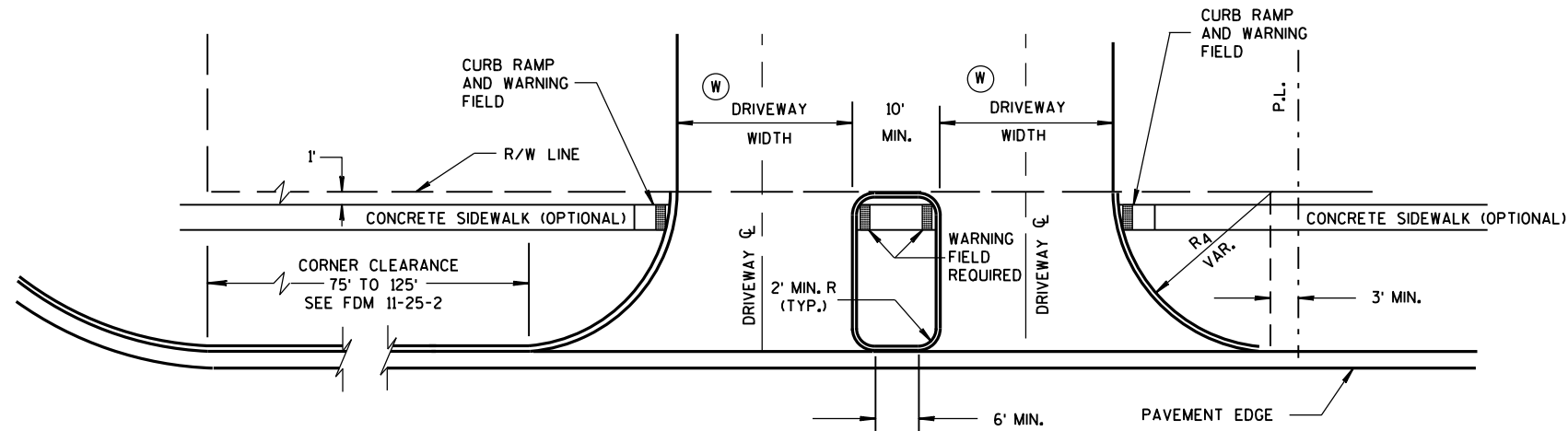
CONCRETE SURFACE DRAINS & ASPHALTIC FLUMES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
9-4-08 /S/ Jerry H. Zogg
DATE ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA



**SKewed DRIVEWAY DETAILS
(COMMERCIAL AND NON-COMMERCIAL)
SIDEWALK NOT SHOWN**



**DRIVEWAY LOCATION AND SPACING DETAILS
SIDEWALK SHOWN**

NOTES

A MAXIMUM RADIUS OF 10 FEET SHALL BE USED FOR NON-COMMERCIAL PRIVATE ENTRANCES. RADII FOR COMMERCIAL DRIVEWAYS SHALL BE DETERMINED BY THE ENGINEER BASED ON TRAFFIC AND DRIVEWAY PERMIT RESTRICTIONS.

THE MINIMUM ANGLE OF INTERSECTION BETWEEN THE DRIVEWAY AND HIGHWAY CENTERLINES SHALL BE 70°.

ALL CURVILINEAR PRIVATE ENTRANCE OUTLINES SHALL BE CONTAINED WITHIN THE HIGHWAY R/W.

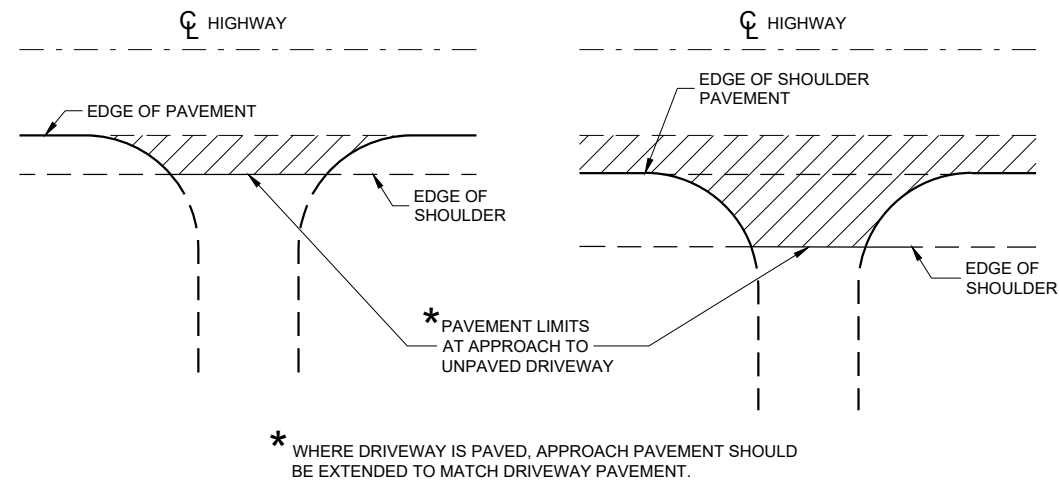
NO DRIVEWAY SHALL BE BUILT WITHIN 3 FEET OF THE PROPERTY LINE EXCEPT FOR EXISTING JOINT DRIVEWAY SHARED BY TWO OWNERS.

- Ⓜ DRIVEWAY WIDTHS:
 COMMERCIAL - 35' MAX., 16' MIN.
 RESIDENTIAL AND NON-COMMERCIAL - 24' MAX., 12' MIN.

**DRIVEWAYS WITH
CURB & GUTTER RETURNS**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

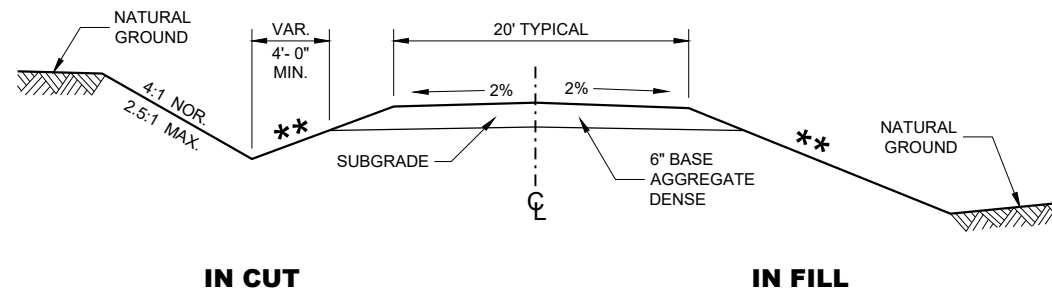
APPROVED
 December, 2016 /s/ Rodney Taylor
 DATE ROADWAY STANDARDS DEVELOPMENT
 FHWA UNIT SUPERVISOR



PLAN VIEW
(UNPAVED SHOULDER ON HIGHWAY)

PLAN VIEW
(PAVED SHOULDER ON HIGHWAY)

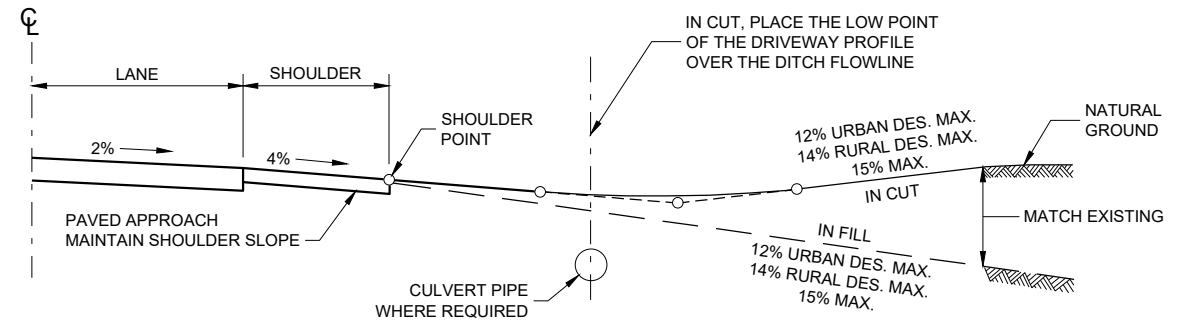
**RURAL DRIVEWAY INTERSECTION DETAIL
(NO CURB AND GUTTER OR SIDEWALK)**



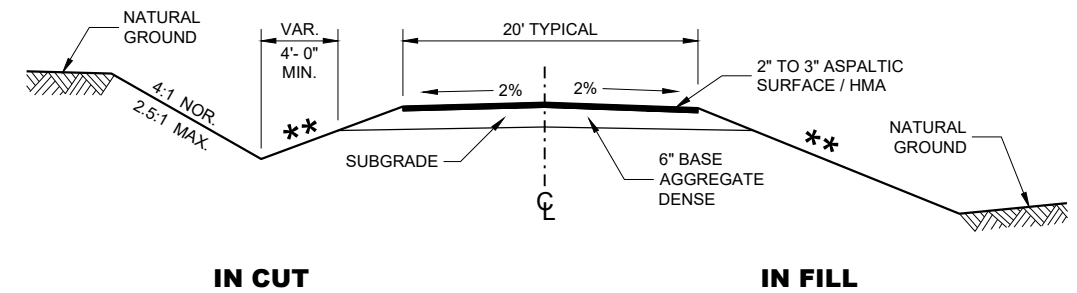
**TYPICAL CROSS SECTION FOR
PRIVATE DRIVE OR FIELD ENTRANCE
AGGREGATE SURFACE**

** SLOPE CAN VARY WITH SPEED. SEE 11-45-30.6.2

POSTED SPEED MPH	MAX. SLOPE
<35	4:1
≥ 35 TO < 60	6:1
≥60	10:1



TYPICAL DRIVEWAY PROFILES



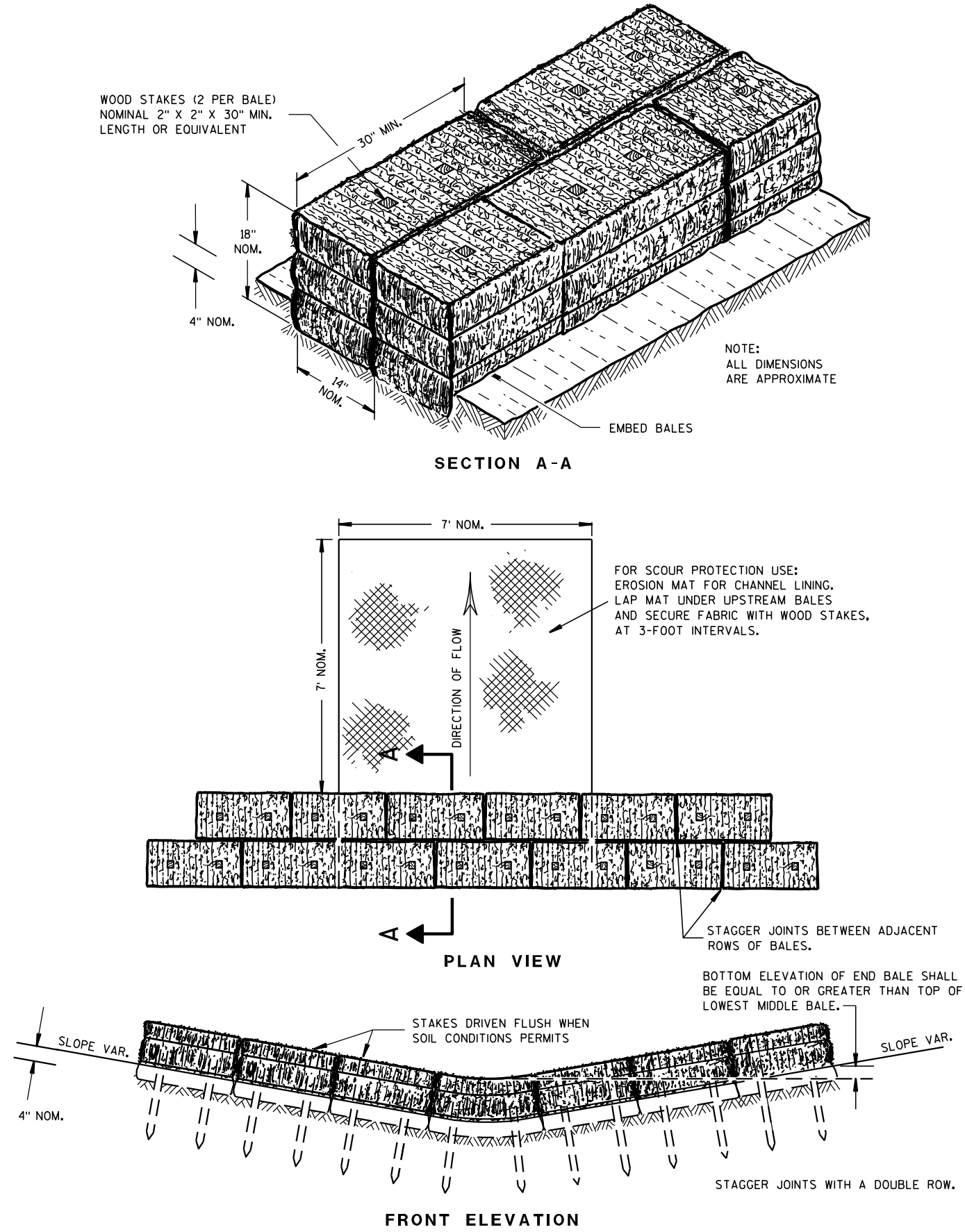
**TYPICAL CROSS SECTION FOR
PRIVATE DRIVE OR FIELD ENTRANCE
ASPHALTIC SURFACE**

DRIVEWAYS WITHOUT CURB AND GUTTER

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
December 2017 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR

FHWA

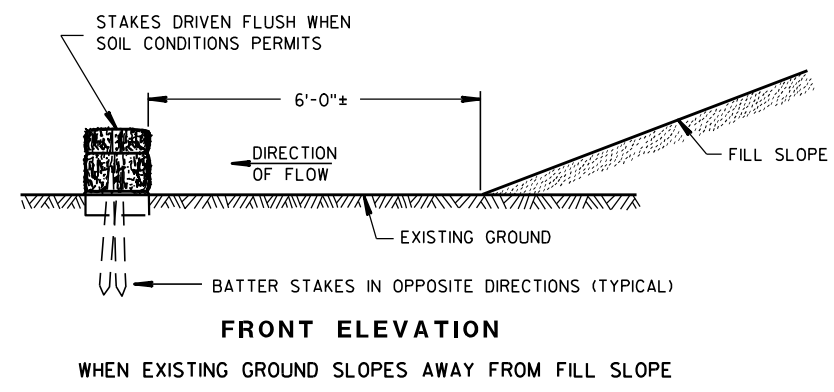
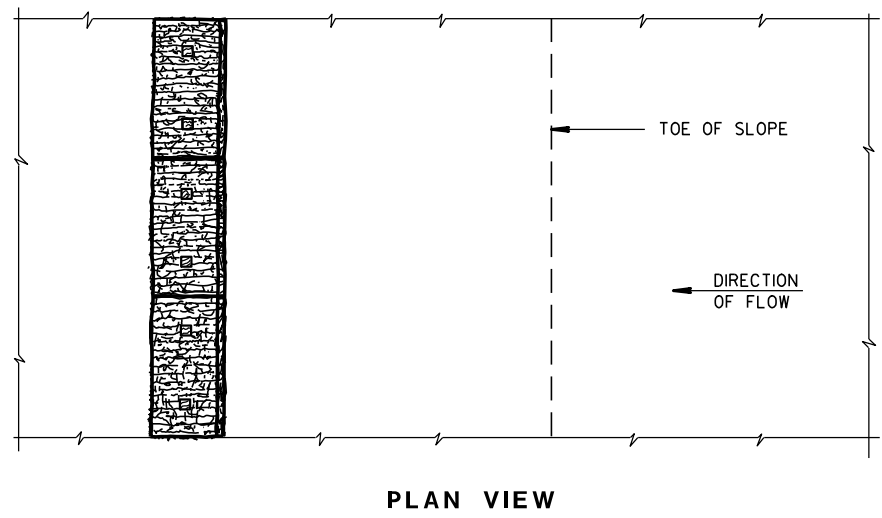
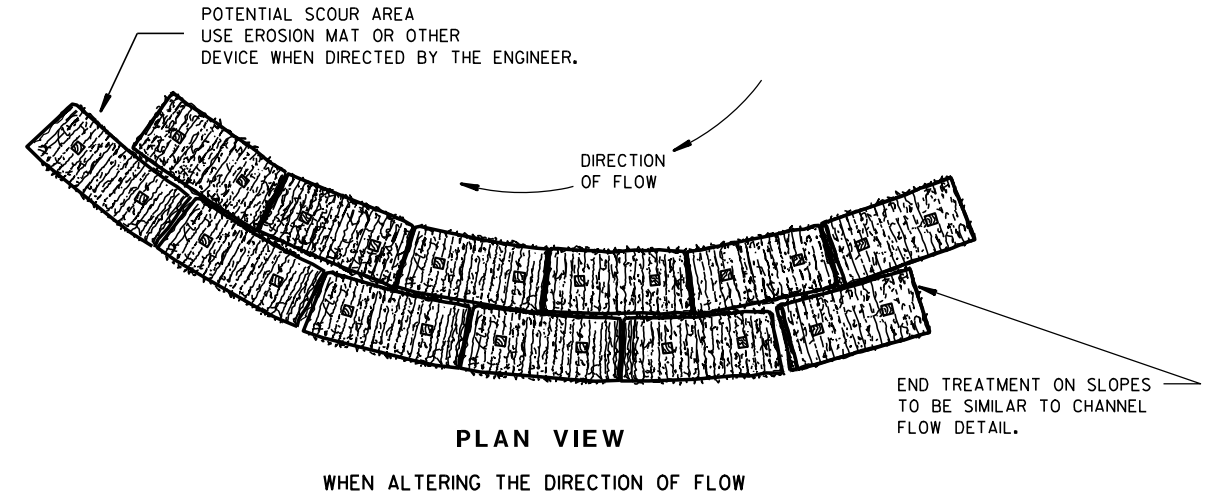


TEMPORARY DITCH CHECK USING EROSION BALES ①

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

- ① TEMPORARY DITCH CHECKS EITHER EROSION BALES OR MANUFACTURED SHALL BE PAID FOR UNDER THE BID ITEM OF TEMPORARY DITCH CHECK. THE DEPARTMENT WILL NOT PAY FOR TEMPORARY DITCH CHECKS CONSTRUCTED OF A SINGLE ROW OF EROSION BALES.

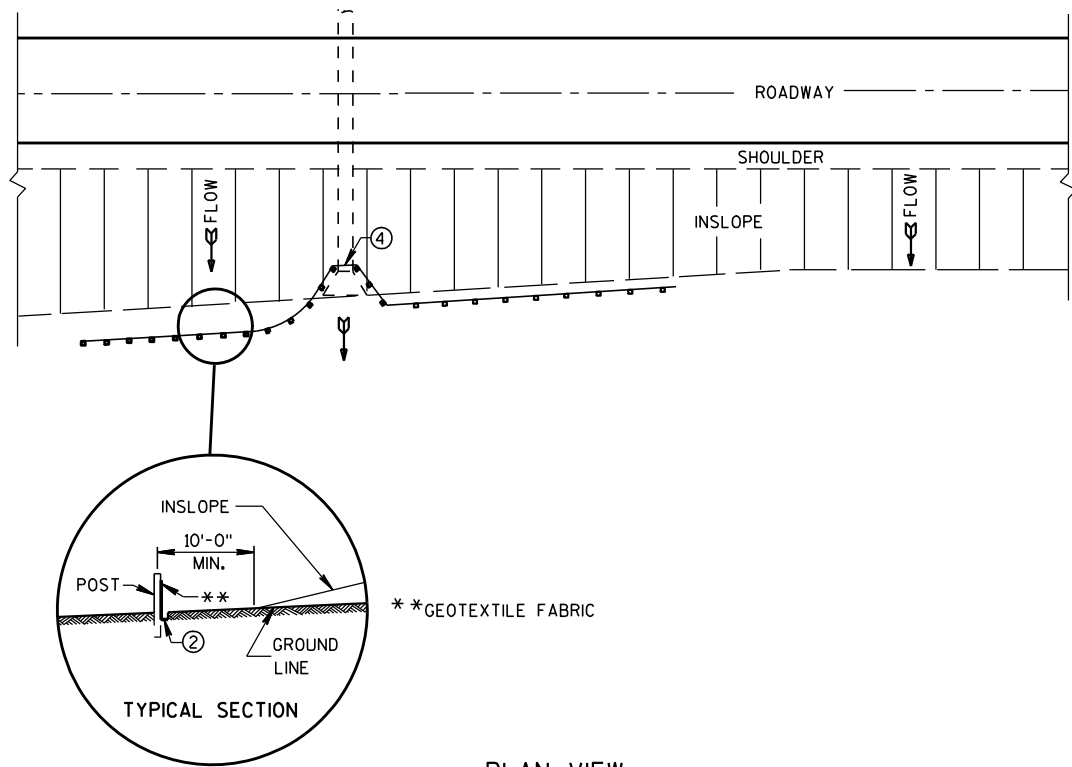


EROSION BALES FOR SHEET FLOW

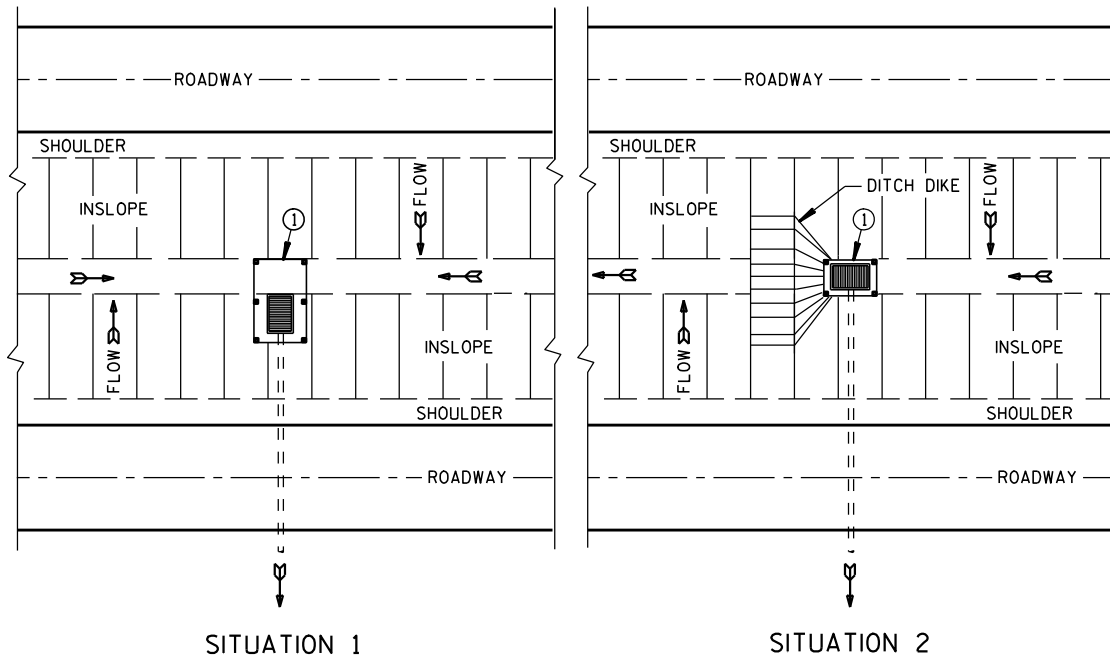
TYPICAL INSTALLATIONS OF EROSION BALES / TEMPORARY DITCH CHECKS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
 6/04/02 /S/ Beth Canestra
 DATE CHIEF ROADWAY DEVELOPMENT ENGINEER
 FHWA



PLAN VIEW
TYPICAL APPLICATION OF SILT FENCE

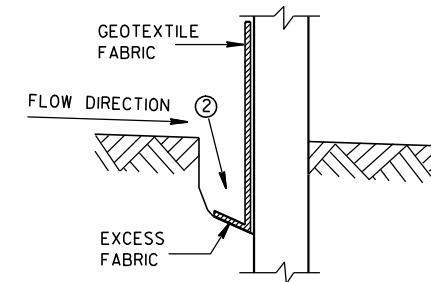


SITUATION 1 SITUATION 2
PLAN VIEW
SILT FENCE AT MEDIAN SURFACE DRAINS

GENERAL NOTES

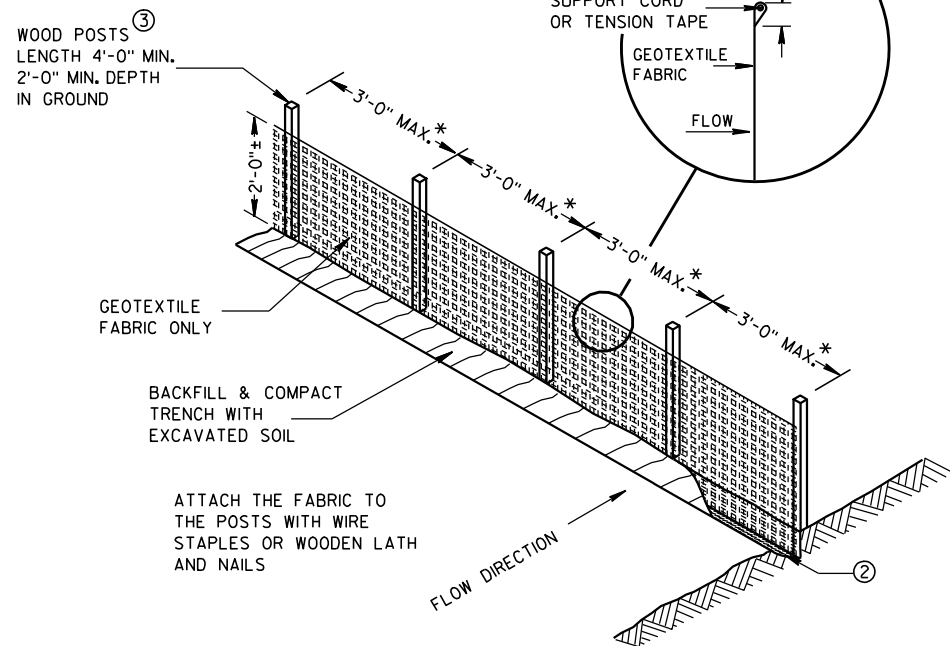
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

- ① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- ③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1 1/8" X 1 1/8" OF OAK OR HICKORY.
- ④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- ⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



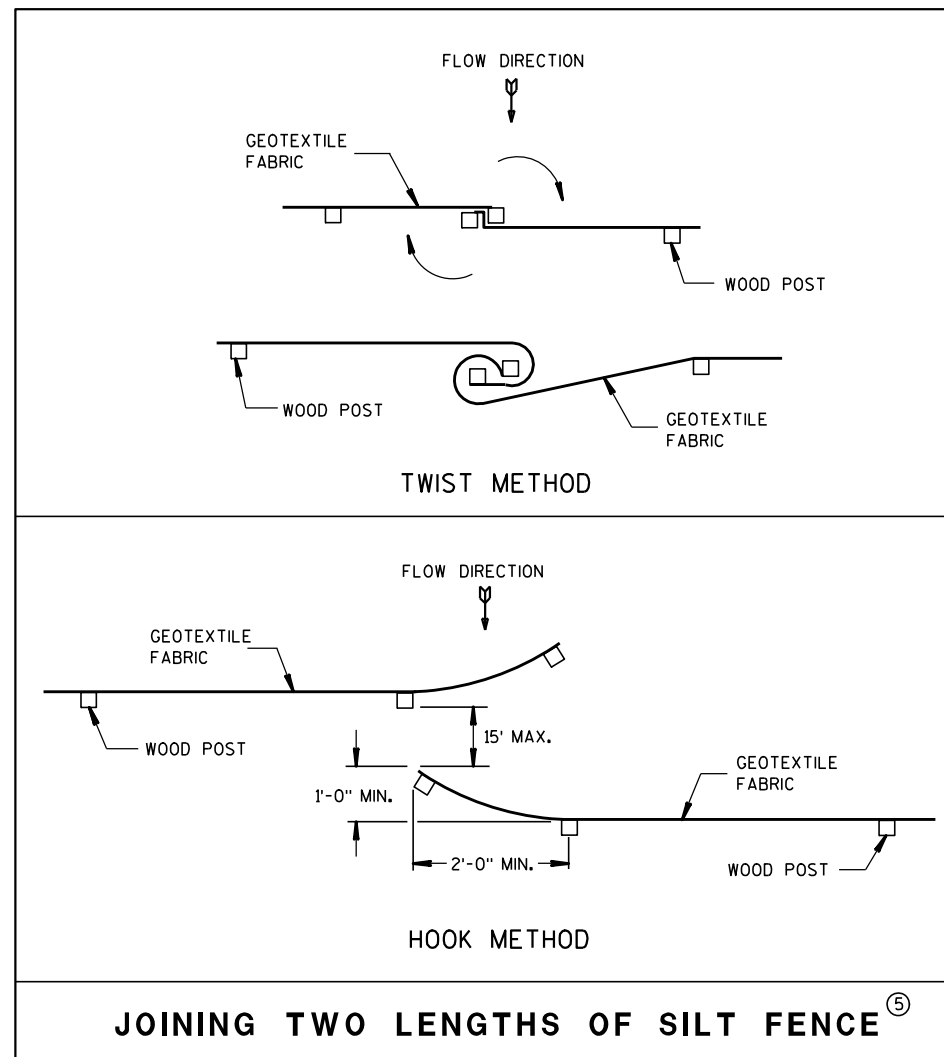
TRENCH DETAIL

NOTE: ADDITIONAL POST DEPTH OR TIE BACKS MAY BE REQUIRED IN UNSTABLE SOILS

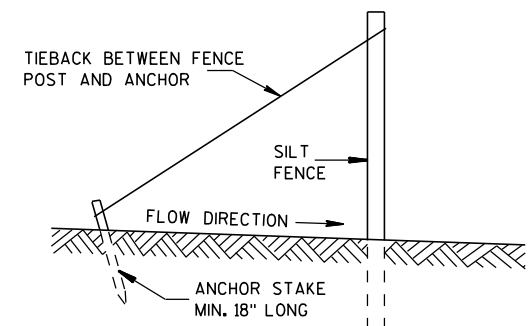


SILT FENCE

* NOTE: 8'-0" POST SPACING ALLOWED IF A WOVEN GEOTEXTILE FABRIC IS USED.



JOINING TWO LENGTHS OF SILT FENCE ⑤

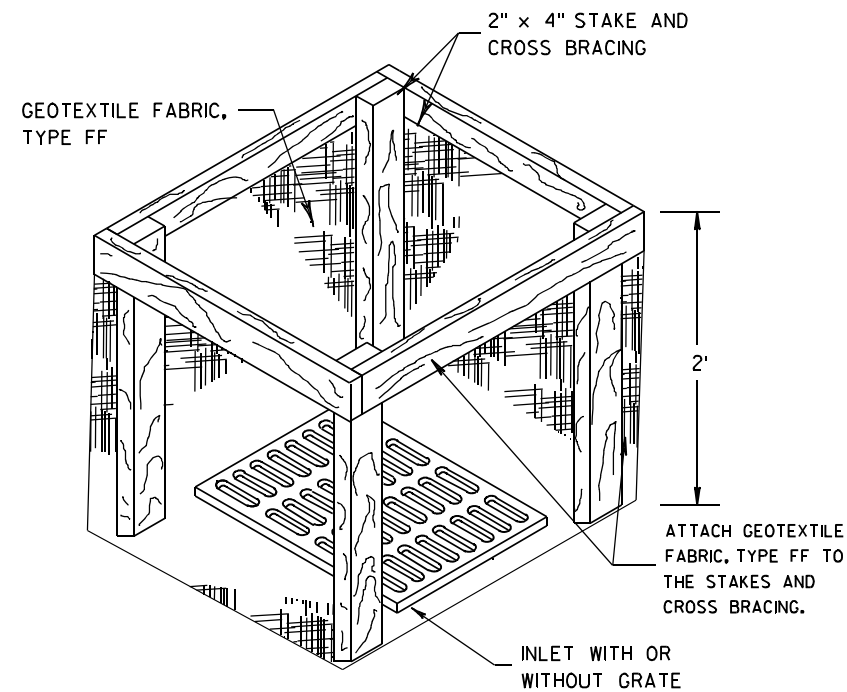
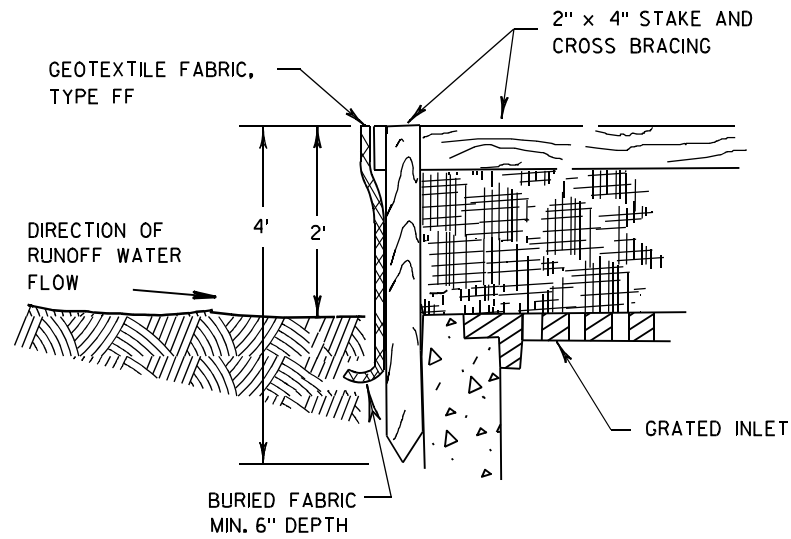


SILT FENCE TIE BACK
(WHEN REQUIRED BY THE ENGINEER)

SILT FENCE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
4-29-05 /S/ Beth Canestra
DATE CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA



INLET PROTECTION, TYPE A

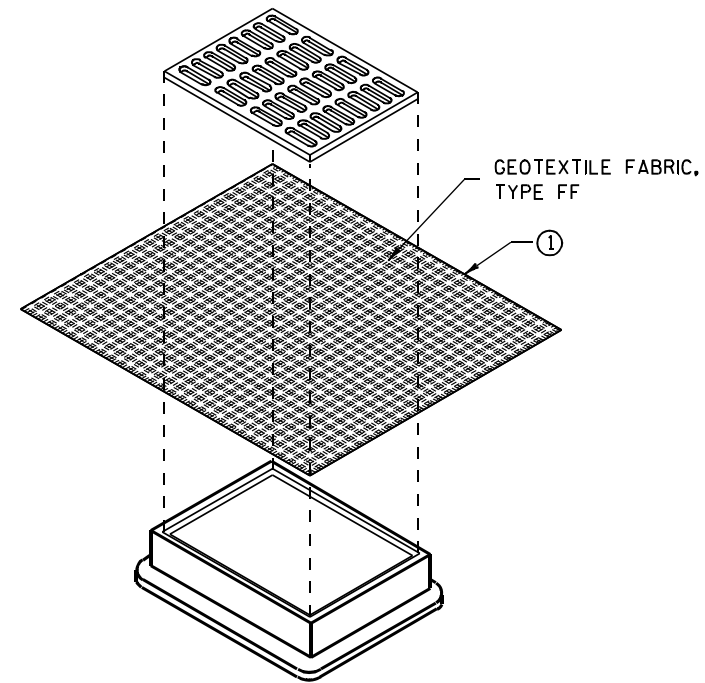
GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

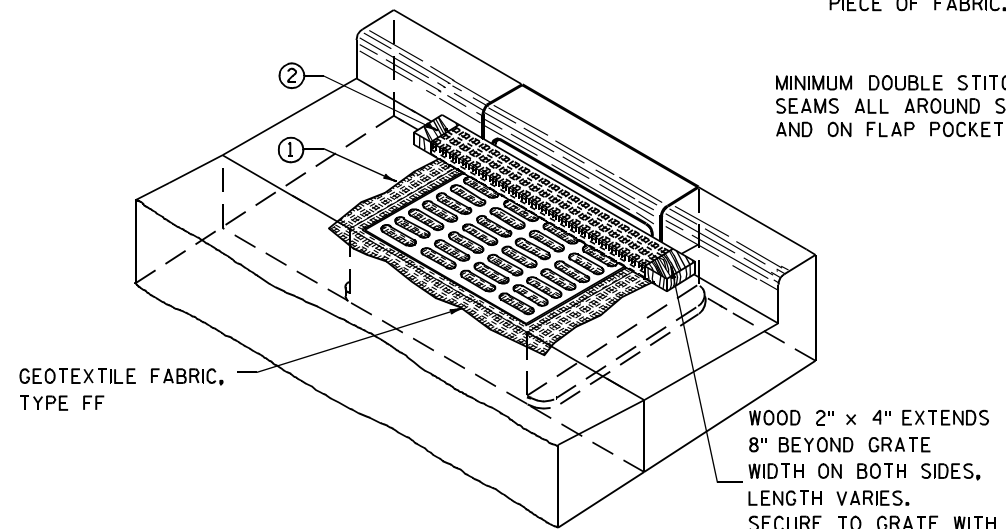
WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- ① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- ② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- ③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



**INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)**

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE C (WITH CURB BOX)

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

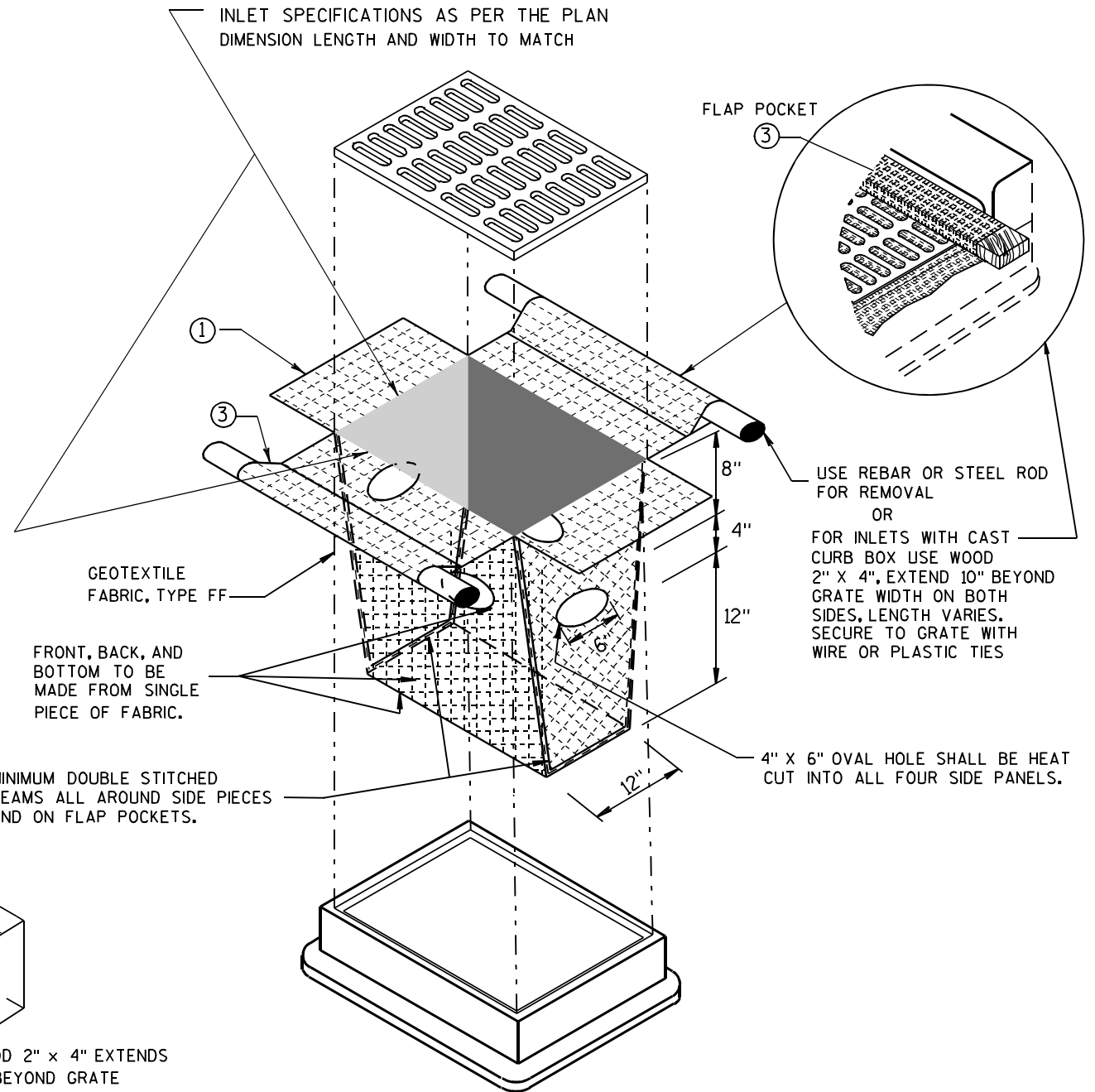
THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

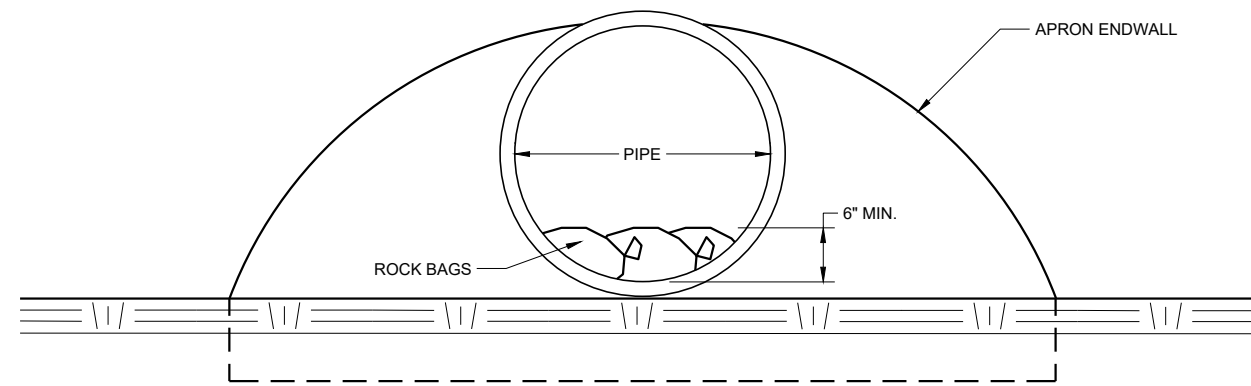
THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.



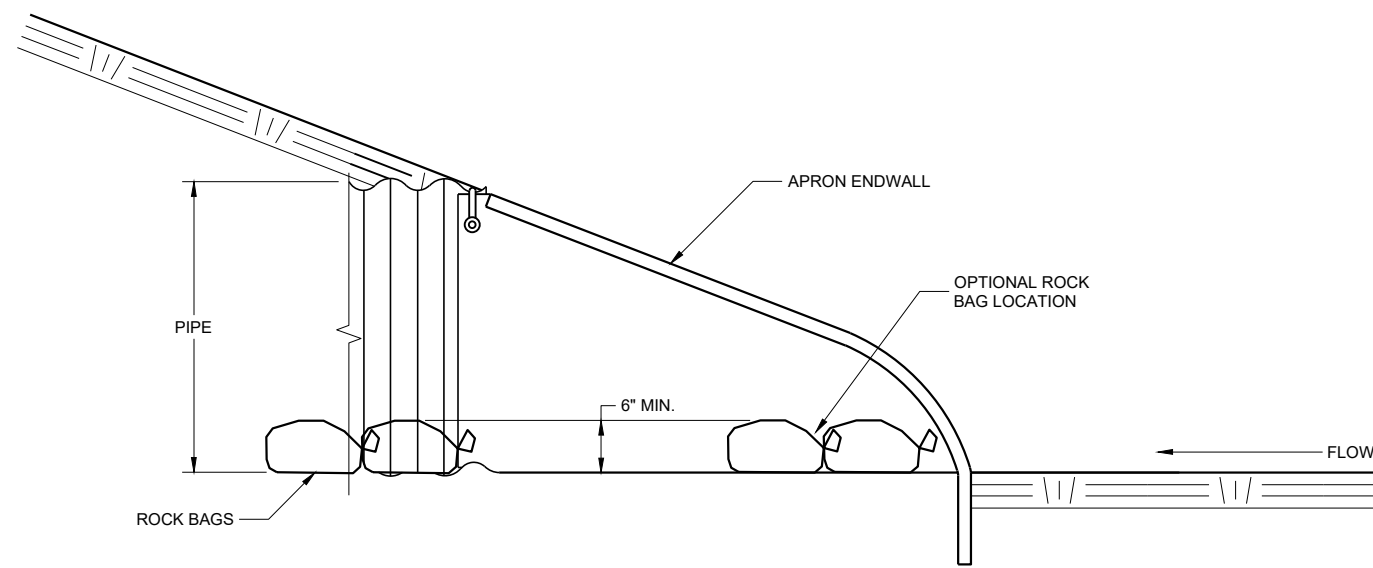
INLET PROTECTION, TYPE D

(CAN BE INSTALLED IN ANY INLET TYPE WITH OR WITHOUT A CURB BOX AS PER NOTE ②)

INLET PROTECTION TYPE A, B, C, AND D	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 10/16/02 DATE	/S/ Beth Conestra CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA	



END VIEW



SIDE VIEW

CULVERT PIPE CHECK
(INSTALL ON INLET END ONLY)

CULVERT PIPE CHECK

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2019 /S/ Daniel Schave
DATE EROSION CONTROL ENGINEER

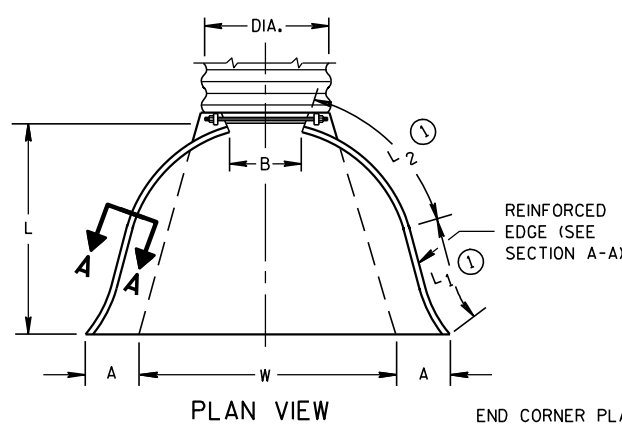
FHWA

METAL APRON ENDWALLS											
PIPE DIA. (IN.)	MIN. THICK. (Inches)		DIMENSIONS (Inches)							APPROX. SLOPE	BODY
	STEEL	ALUM.	A (±1")	B (MAX.)	H (±1")	L (±1 1/2")	L1	L2	W (±2")		
12	.064	.060	6	6	6	21	12	17 1/2	24	2 1/2 to 1	1 Pc.
15	.064	.060	7	8	6	26	14	21 3/4	30	2 1/2 to 1	1 Pc.
18	.064	.060	8	10	6	31	15	28 1/4	36	2 1/2 to 1	1 Pc.
21	.064	.060	9	12	6	36	18	29 5/8	42	2 1/2 to 1	1 Pc.
24	.064	.075	10	13	6	41	18	37 1/4	48	2 1/2 to 1	1 Pc.
30	.079	.075	12	16	8	51	18	52 1/4	60	2 1/2 to 1	1 Pc.
36	.079	.105	14	19	9	60	24	59 3/4	72	2 1/2 to 1	2 Pc.
42	.109	.105	16	22	11	69	24	75 5/8	84	2 1/2 to 1	2 Pc.
48	.109	.105	18	27	12	78	24	81	90	2 1/4 to 1	3 Pc.
54	.109	.105	18	30	12	84	30	85 1/2	102	2 1/4 to 1	3 Pc.
60	.109x	.105x	18	33	12	87	—	—	114	2 to 1	3 Pc.
66	.109x	.105x	18	36	12	87	—	—	120	2 to 1	3 Pc.
72	.109x	.105x	18	39	12	87	—	—	126	2 to 1	3 Pc.
78	.109x	.105x	18	42	12	87	—	—	132	1 1/2 to 1	3 Pc.
84	.109x	.105x	18	45	12	87	—	—	138	1 1/2 to 1	3 Pc.
90	.109x	.105x	18	37	12	87	—	—	144	1 1/2 to 1	3 Pc.
96	.109x	.105x	18	35	12	87	—	—	150	1 1/2 to 1	3 Pc.

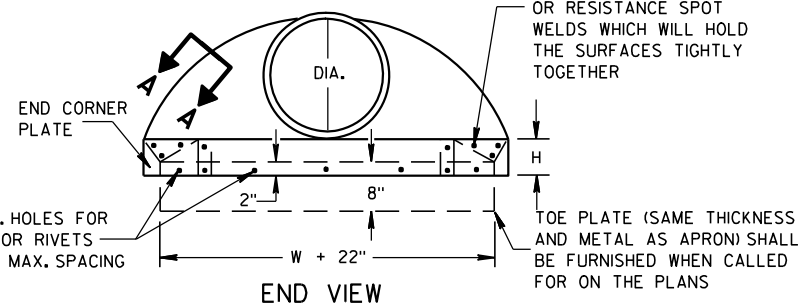
* EXCEPT CENTER PANEL SEE GENERAL NOTES

REINFORCED CONCRETE APRON ENDWALLS									
PIPE DIA. (IN.)	DIMENSIONS (Inches)							APPROX. SLOPE	
	T	A	B	C	D	E	G		
12	2	4	24	48 1/8	72 1/8	24	2	3 to 1	
15	2 1/4	6	27	46	73	30	2 1/4	3 to 1	
18	2 1/2	9	27	46	73	36	2 1/2	3 to 1	
21	2 3/4	9	36	37 1/2	73 1/2	42	2 3/4	3 to 1	
24	3	9 1/2	43 1/2	30	73 1/2	48	3	3 to 1	
27	3 1/4	10 1/2	49 1/2	24	73 1/2	54	3 1/4	3 to 1	
30	3 1/2	12	54	19 3/4	73 1/2	60	3 1/2	3 to 1	
36	4	15	63	34 3/4	97 3/4	72	4	3 to 1	
42	4 1/2	21	63	35	98	78	4 1/2	3 to 1	
48	5	24	72	26	98	84	5	3 to 1	
54	5 1/2	27	65	33 1/4-35	98 1/4-100	90	5 1/2	2 1/2 to 1	
60	6	30-35	60	39	99	96	5	2 to 1	
66	6 1/2	30-35	72-78	21-27	99	102	5 1/2	2 to 1	
72	7	30-35	78	21	99	108	6	2 to 1	
78	7 1/2	30-35	78	21	99	114	6 1/2	2 to 1	
84	8	36	90 1/2	21	111 1/2	120	6 1/2	1 1/2 to 1	
90	8 1/2	41	87 1/2	24	111 1/2	132	6 1/2	1 1/2 to 1	

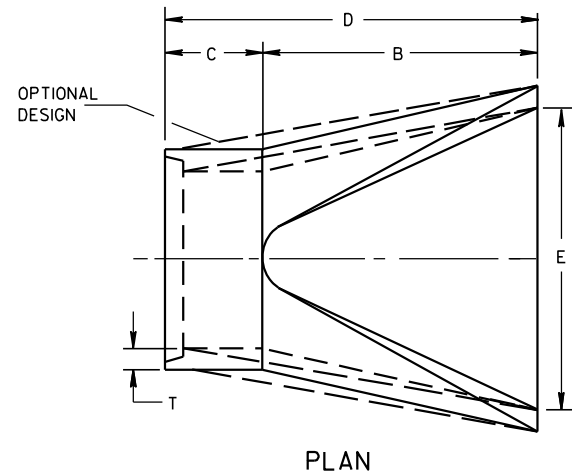
* MINIMUM
** MAXIMUM



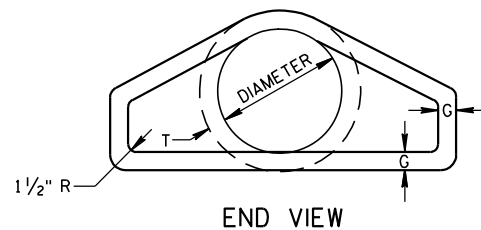
END CORNER PLATES MAY BE FASTENED TO APRON PROPER BY BOLTS, RIVETS, OR RESISTANCE SPOT WELDS WHICH WILL HOLD THE SURFACES TIGHTLY TOGETHER



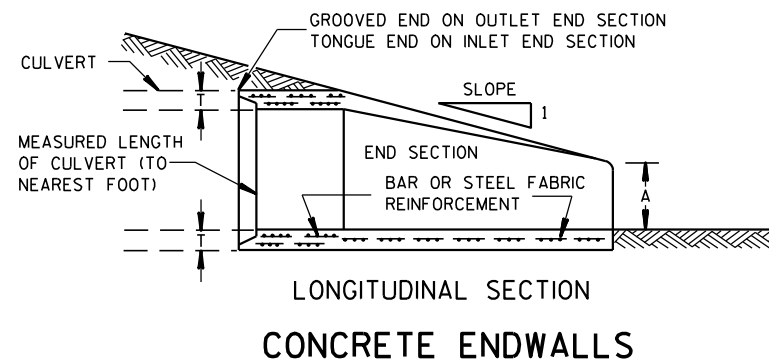
SIDE ELEVATION
METAL ENDWALLS



PLAN

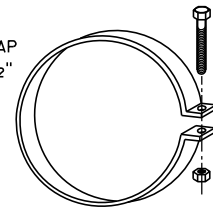


END VIEW

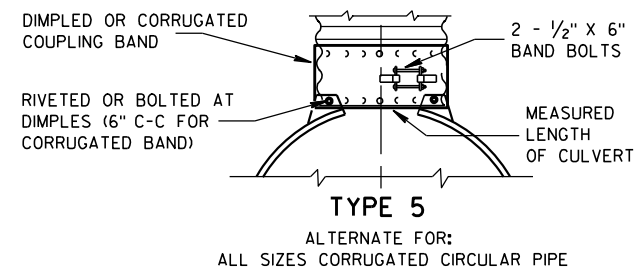
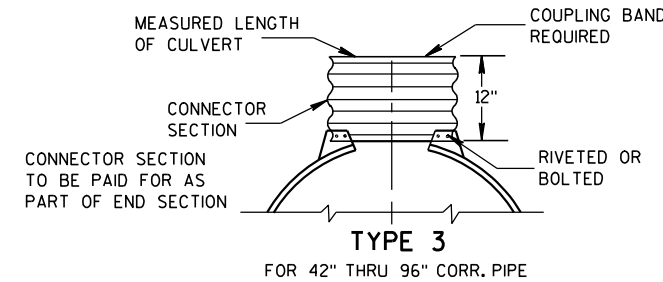
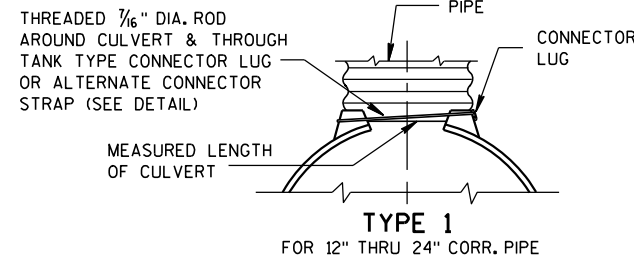


LONGITUDINAL SECTION
CONCRETE ENDWALLS

1" WIDE, 12 GA. (0.109" THICK) GALVANIZED STRAP WITH STANDARD 6" X 1/2" BAND BOLT AND NUT



ALTERNATE FOR TYPE 1 CONNECTION
END SECTION CONNECTOR STRAP



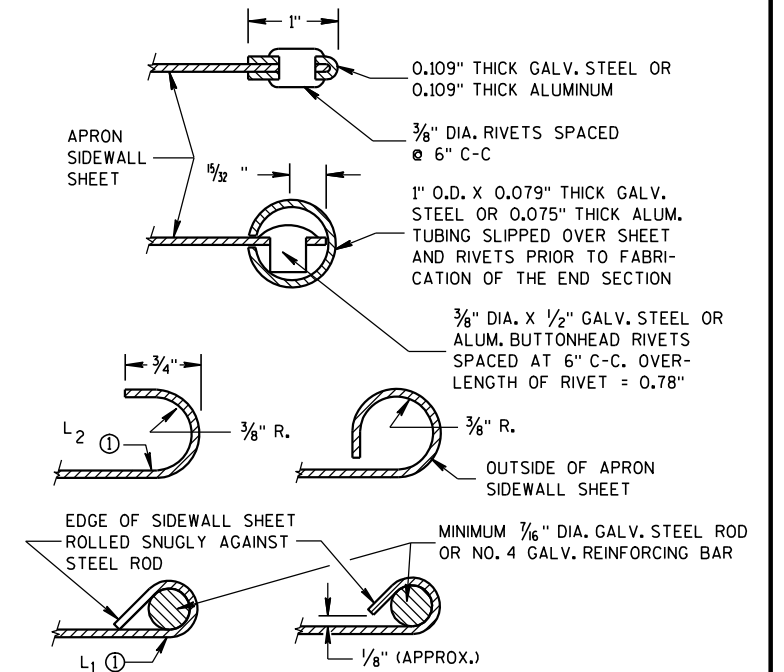
NOTE: DIMPLED BAND FITS OVER OUTSIDE OF ENDWALL, AND CORRUGATED BAND FITS INSIDE ENDWALL. DIMPLED BAND MAY BE USED WITH HELICALLY CORRUGATED PIPE.

FOR CIRCUMFERENTIALLY CORRUGATED PIPE USE ENDWALL CONNECTION DETAILS 1, 2, 3 OR 5 AS APPLICABLE.

FOR HELICALLY CORRUGATED PIPE USE ENDWALL CONNECTION DETAILS 1, 2 OR 5.

FOR HELICALLY CORRUGATED PIPES WITH TWO CIRCUMFERENTIAL CORRUGATIONS AT EACH END USE ENDWALL CONNECTION DETAILS 1, 2 OR 3.

CONNECTION DETAILS



SECTION A-A

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

CONCRETE CULVERT ENDWALLS MAY NOT BE USED WITH GALVANIZED STEEL OR ALUMINUM CULVERT PIPE OR VICE VERSA. GALVANIZED STEEL OR ALUMINUM ENDWALLS SHALL NORMALLY BE INSTALLED ON CULVERT PIPE OF THE SAME METAL.

ALL THREE PIECE STEEL APRON ENDWALLS FOR 60" DIAMETER PIPE AND LARGER SHALL HAVE 0.109" SIDES AND 0.138" CENTER PANELS. ALL THREE PIECE ALUMINUM APRON ENDWALLS FOR 60" DIAMETER PIPE AND LARGER SHALL HAVE 0.105" SIDES AND 0.134" CENTER PANELS. THE WIDTH OF CENTER PANELS SHALL BE GREATER THAN 20 PERCENT OF THE PIPE PERIMETER.

LAP SEAMS SHALL BE TIGHTLY JOINED BY GALVANIZED RIVETS OR BOLTS FOR STEEL UNITS AND ALUMINUM RIVETS AND BOLTS FOR ALUMINUM UNITS. FOR THE 60" THROUGH 96" DIAMETER APRON ENDWALL SIZES, THE REINFORCED EDGES AND CENTER PANEL SEAMS SHALL BE FURTHER REINFORCED WITH GALVANIZED STEEL OR ALUMINUM STIFFENER ANGLES. THE ANGLES SHALL BE ATTACHED BY GALVANIZED NUTS AND BOLTS FOR STEEL UNITS AND ALUMINUM NUTS AND BOLTS FOR ALUMINUM UNITS.

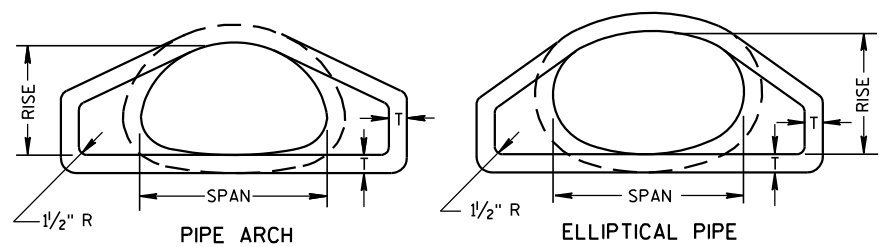
WHERE TWO OR MORE PIPES WITH APRON ENDWALLS ARE LAID ADJACENT TO EACH OTHER, THEY SHALL BE SEPARATED BY A DISTANCE SUFFICIENT TO PROVIDE A MINIMUM CLEARANCE OF 6 INCHES BETWEEN APRON ENDWALLS.

① FOR PIPE SIZES UP TO 60" DIAMETER, A 180° ROLLED EDGE MAY BE USED INSTEAD OF STEEL ROD REINFORCEMENT. SEE SECTION A-A.

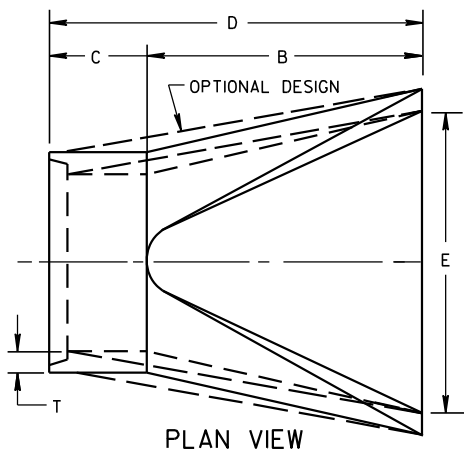
APRON ENDWALLS FOR
CULVERT PIPE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

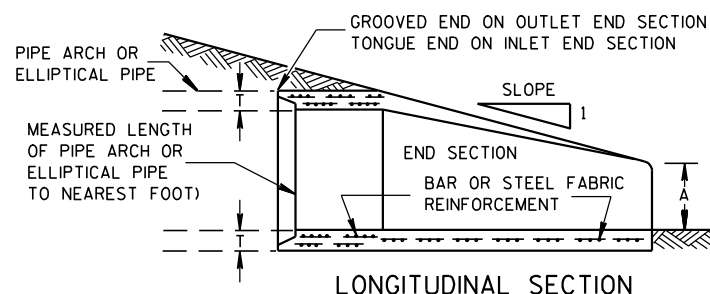
APPROVED
11/30/94 DATE /S/ Rory L. Rhinesmith
DATE CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA



END VIEW



PLAN VIEW



LONGITUDINAL SECTION

CONCRETE ENDWALLS

2- 2 2/3" X 1/2" CORRUGATIONS

EQUIV. DIA. (Inches)	(Inches)		MIN. THICK. (Inches)		DIMENSIONS (Inches)							APPROX. SLOPE	BODY
	SPAN	RISE	STEEL	ALUM.	A (±1")	B (MAX.)	H (±1")	L (±1 1/2")	L1 (⊙)	L2 (⊙)	W (±2")		
15	17	13	.064	.060	7	9	6	19	14	16	30	2 1/2 to 1	1 Pc.
18	21	15	.064	.060	7	10	6	23	14	19 3/8	36	2 1/2 to 1	1 Pc.
21	24	18	.064	.060	8	12	6	28	18	21 3/4	42	2 1/2 to 1	1 Pc.
24	28	20	.064	.060	9	14	6	32	18	27 1/2	48	2 1/2 to 1	1 Pc.
30	35	24	.079	.075	10	16	6	39	18	37 5/8	60	2 1/2 to 1	1 Pc.
36	42	29	.079	.075	12	18	8	46	24	45 3/8	75	2 1/2 to 1	1 Pc.
42	49	33	.109	.105	13	21	9	53	24	54 3/4	85	2 1/2 to 1	2 Pc.
48	57	38	.109	.105	18	26	12	63	24	68	90	2 1/2 to 1	3 Pc.
54	64	43	.109	.105	18	30	12	70	24	72 3/4	102	2 1/4 to 1	3 Pc.
60	71	47	.109*	.105*	18	33	12	77	30	82 1/4	114	2 1/4 to 1	3 Pc.
66	77	52	.109*	.105*	18	36	12	77	—	—	126	2 to 1	3 Pc.
72	83	57	.109*	.105*	18	39	12	77	—	—	138	2 to 1	3 Pc.

3" X 1" CORRUGATIONS

EQUIV. DIA. (Inches)	(Inches)		MIN. THICK. (Inches)		DIMENSIONS (Inches)							APPROX. SLOPE	BODY
	SPAN	RISE	STEEL	ALUM.	A (±1")	B (MAX.)	H (±1")	L (±1 1/2")	L1 (⊙)	L2 (⊙)	W (±2")		
48	53	41	.109	.105	18	26	12	63	24	72 3/4	90	2 1/2 to 1	2 Pc.
54	60	46	.109	.105	18	30	12	70	30	82 1/4	102	2 to 1	2 Pc.
60	66	51	.109*	.105*	18	33	12	77	—	—	114	1 1/2 to 1	3 Pc.
66	73	55	.109*	.105*	18	36	12	77	—	—	126	1 1/2 to 1	3 Pc.
72	81	59	.109*	.105*	18	39	12	77	—	—	138	2 to 1	3 Pc.
78	87	63	.109*	.105*	22	38	12	77	—	—	148	1 1/2 to 1	3 Pc.
84	95	67	.109*	.105*	22	34	12	77	—	—	162	1 1/2 to 1	3 Pc.
90	103	71	.109*	.105*	22	38	12	77	—	—	174	1 1/2 to 1	3 Pc.
96	112	75	.109*	.105*	24	40	12	77	—	—	174	1 1/2 to 1	3 Pc.

NOTE: ALL SPLICES TO BE LAP RIVETED OR BOLTED. * EXCEPT CENTER PANEL SEE GENERAL NOTES

REINFORCED CONCRETE PIPE ARCH

EQUIV. DIA. (Inches)	DIMENSIONS (Inches)								APPROX. SLOPE
	**SPAN	**RISE	T	A	B	C	D	E	
24	29	18	3	8 1/2	39	33	72	48	3 to 1
30	36	22	3 1/2	9 1/2	50	46	96	60	3 to 1
36	44	27	4	11 1/8	60	36	96	72	3 to 1
42	51	31	4 1/2	15 1/8	60	36	96	78	3 to 1
48	58	36	5	21	60	36	96	84	3 to 1
54	65	40	5 1/2	25 1/2	60	36	96	90	3 to 1
60	73	45	6	31	60	36	96	96	3 to 1
72	88	54	7	31	60	39	99	120	2 to 1
84	102	62	8	28 1/2	83	19	102	144	2 to 1

REINFORCED CONCRETE ELLIPTICAL PIPE

EQUIV. DIA. (Inches)	DIMENSIONS (Inches)								APPROX. SLOPE
	**SPAN	**RISE	T	A	B	C	D	E	
24	30	19	3 1/4	8 1/2	39	33	72	48	3 to 1
30	38	24	3 3/4	9 1/2	54	18	72	60	3 to 1
36	45	29	4 1/2	11 1/8	60	24	84	72	2 1/2 to 1
42	53	34	5	15 3/4	60	36	96	78	2 1/2 to 1
48	60	38	5 1/2	21	60	36	96	84	2 1/2 to 1
54	68	43	6	25 1/2	60	36	96	90	2 1/2 to 1
60	76	48	6 1/2	30	60	36	96	96	2 1/2 to 1

**NOMINAL SIZE

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

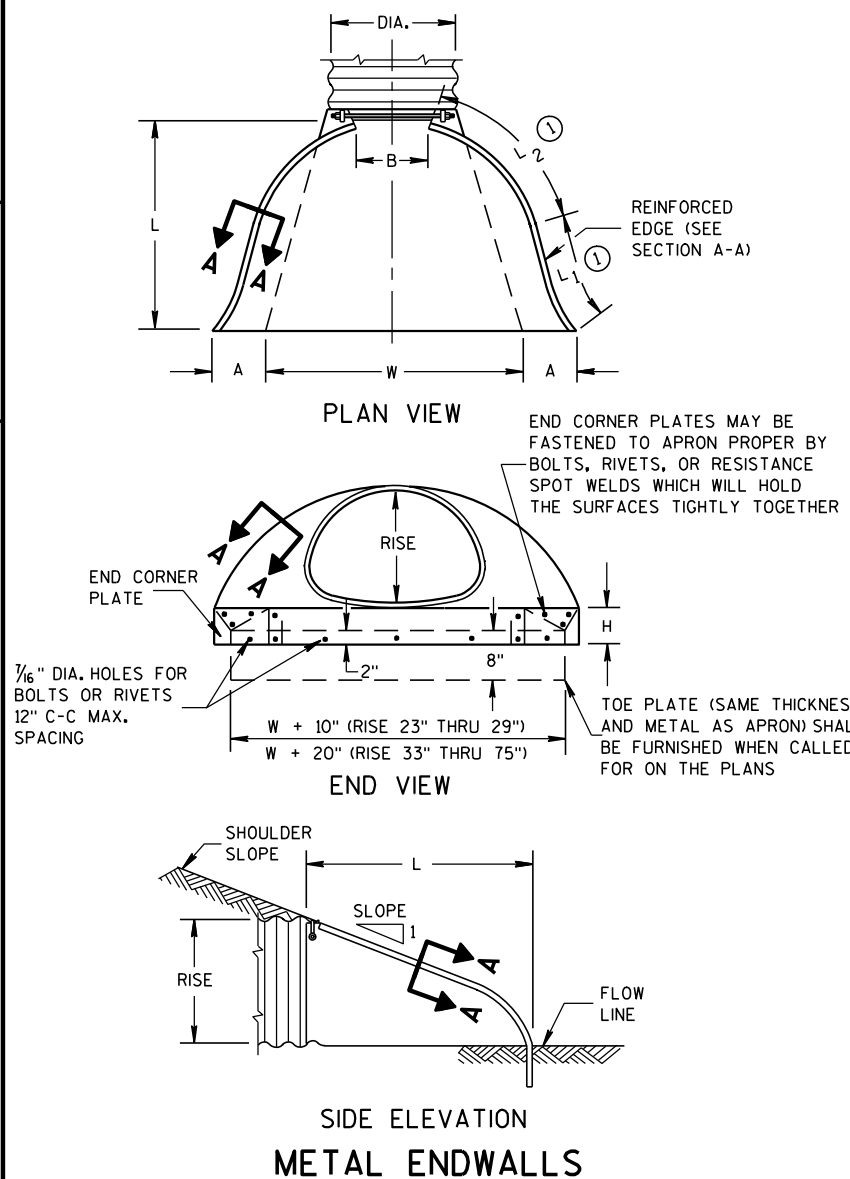
CONCRETE APRON ENDWALLS MAY NOT BE USED WITH GALVANIZED STEEL OR ALUMINUM CULVERT PIPE OR VISE VERSA. GALVANIZED STEEL OR ALUMINUM APRON ENDWALLS SHALL NORMALLY BE INSTALLED ON CULVERT PIPE OF THE SAME METAL.

ALL THREE PIECE STEEL APRON ENDWALLS FOR 66" X 51" PIPE ARCH AND LARGER SHALL HAVE 0.109" SIDES AND 0.138" CENTER PANELS. ALL THREE PIECE ALUMINUM APRON ENDWALLS FOR 66" X 51" PIPE ARCH AND LARGER SHALL HAVE 0.105" SIDES AND 0.134" CENTER PANELS. THE WIDTH OF CENTER PANELS SHALL BE GREATER THAN 20 PERCENT OF THE PIPE ARCH PERIMETER.

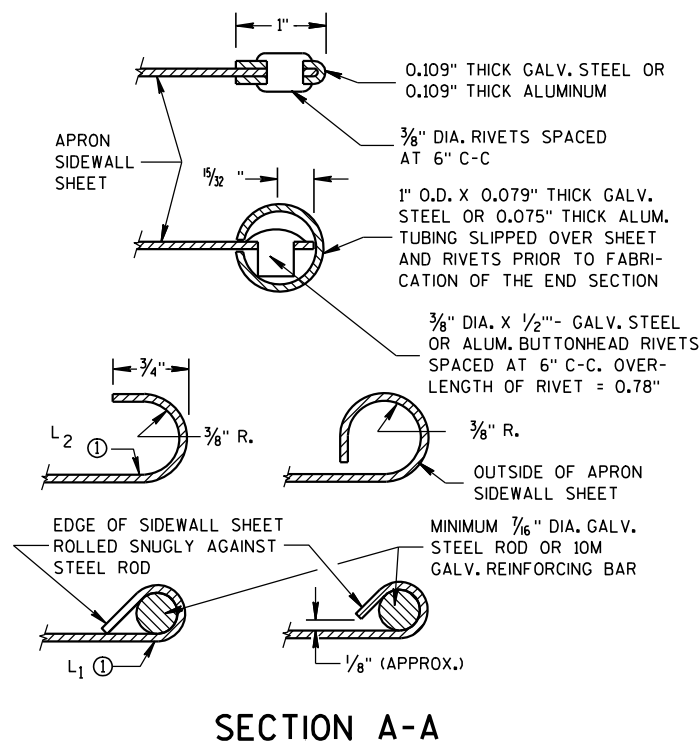
LAP SEAMS SHALL BE TIGHTLY JOINED BY GALVANIZED RIVETS OR BOLTS FOR STEEL UNITS AND ALUMINUM RIVETS AND BOLTS FOR ALUMINUM UNITS. FOR THE 77" X 52" THROUGH 112" X 75" APRON ENDWALL SIZES, THE REINFORCED EDGES AND CENTER PANEL SEAMS SHALL BE FURTHER REINFORCED WITH GALVANIZED STEEL OR ALUMINUM STIFFENER ANGLES. THE ANGLES SHALL BE ATTACHED BY GALVANIZED NUTS AND BOLTS FOR STEEL UNITS AND ALUMINUM NUTS AND BOLTS FOR ALUMINUM UNITS.

WHERE TWO OR MORE PIPES WITH APRON ENDWALLS ARE LAID ADJACENT TO EACH OTHER, THEY SHALL BE SEPARATED BY A DISTANCE SUFFICIENT TO PROVIDE A MINIMUM CLEARANCE OF 6 INCHES BETWEEN APRON ENDWALLS.

Ⓛ FOR PIPE ARCH SIZES UP TO 73" X 55" A 180° ROLLED EDGE MAY BE USED INSTEAD OF STEEL ROD REINFORCEMENT. SEE SECTION A-A.



METAL ENDWALLS

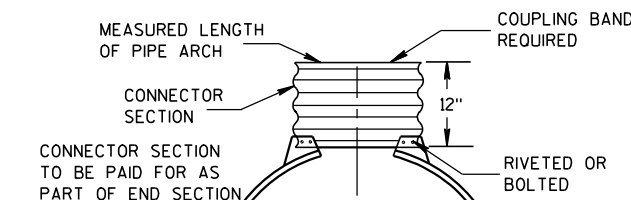


SECTION A-A



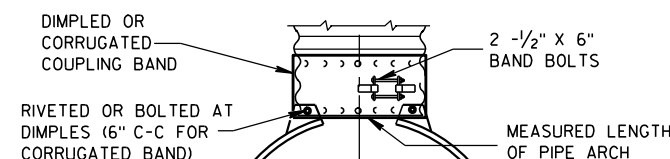
TYPE 2

FOR 17" X 13" THRU 112" X 75" PIPE ARCH



TYPE 3

FOR 64" X 43" THRU 112" X 75" PIPE ARCH



TYPE 5

ALTERNATE FOR: ALL SIZES CORRUGATED PIPE ARCHES

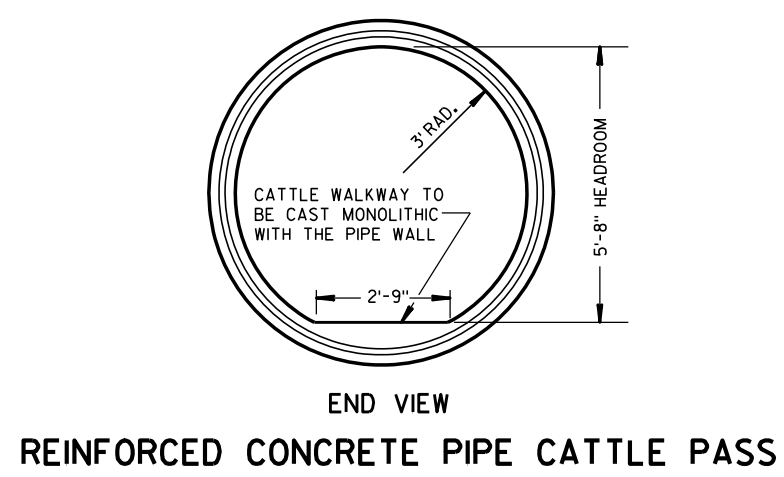
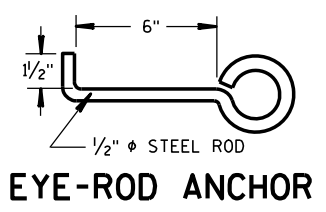
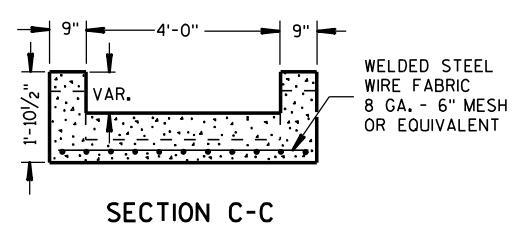
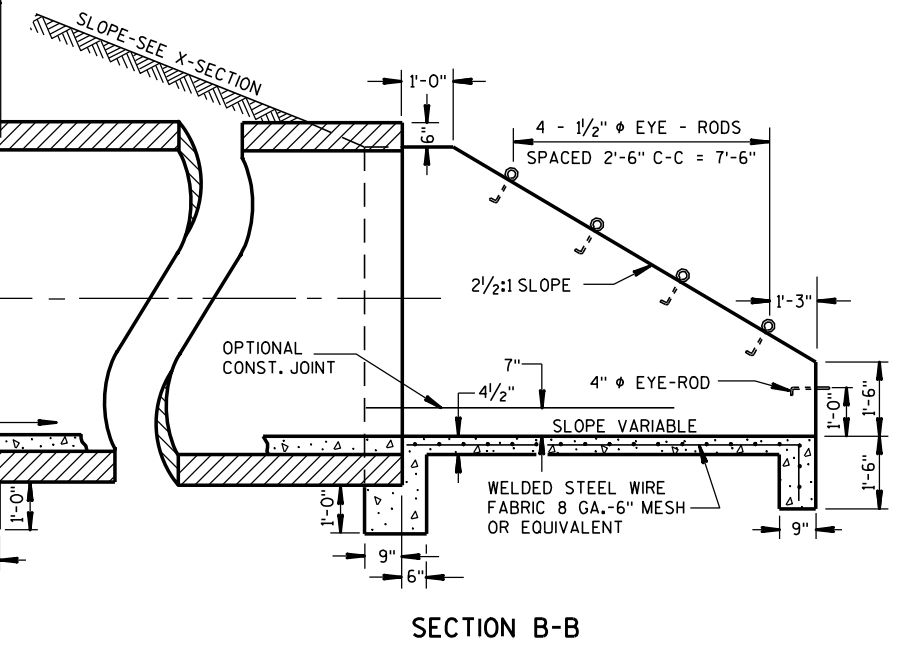
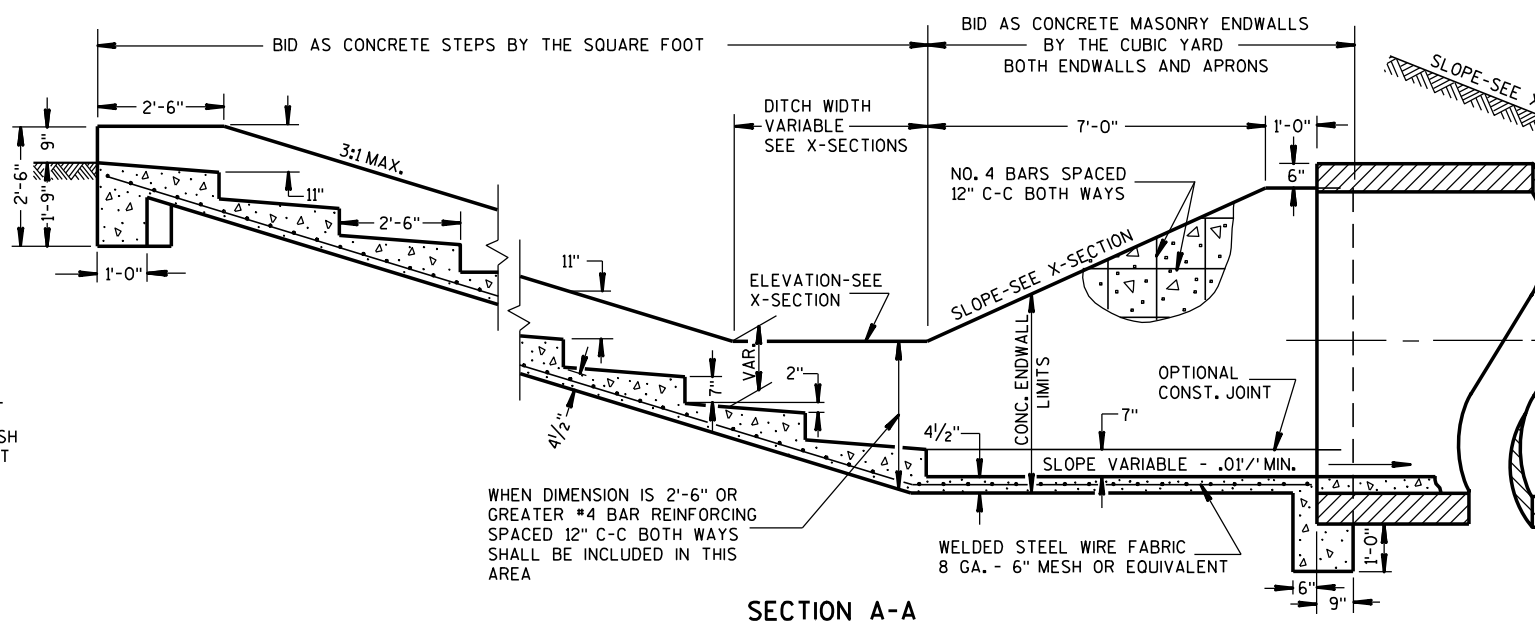
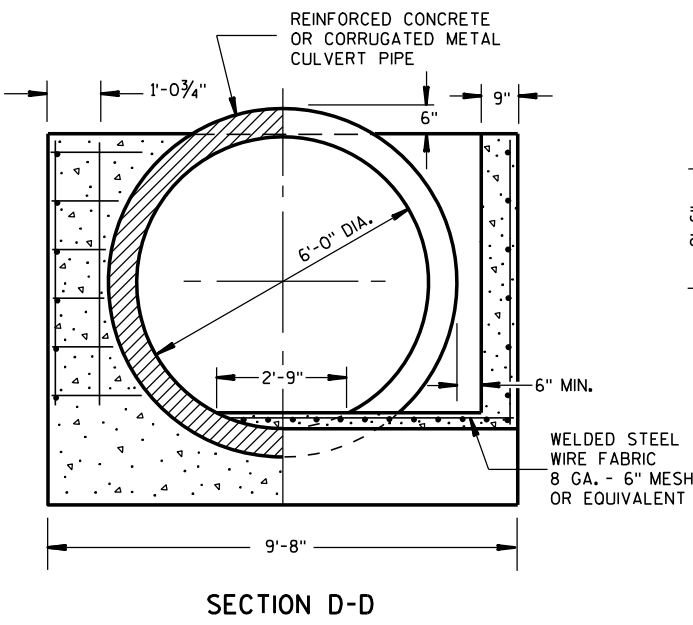
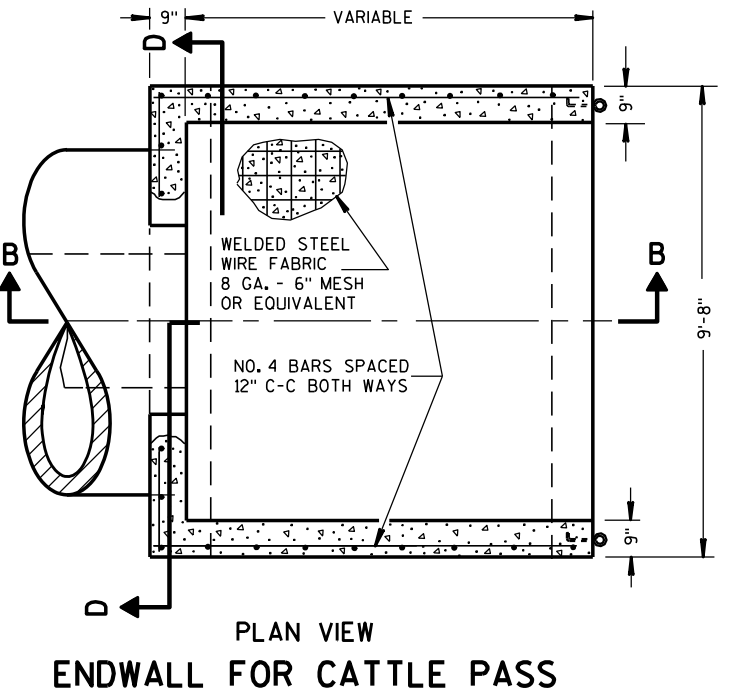
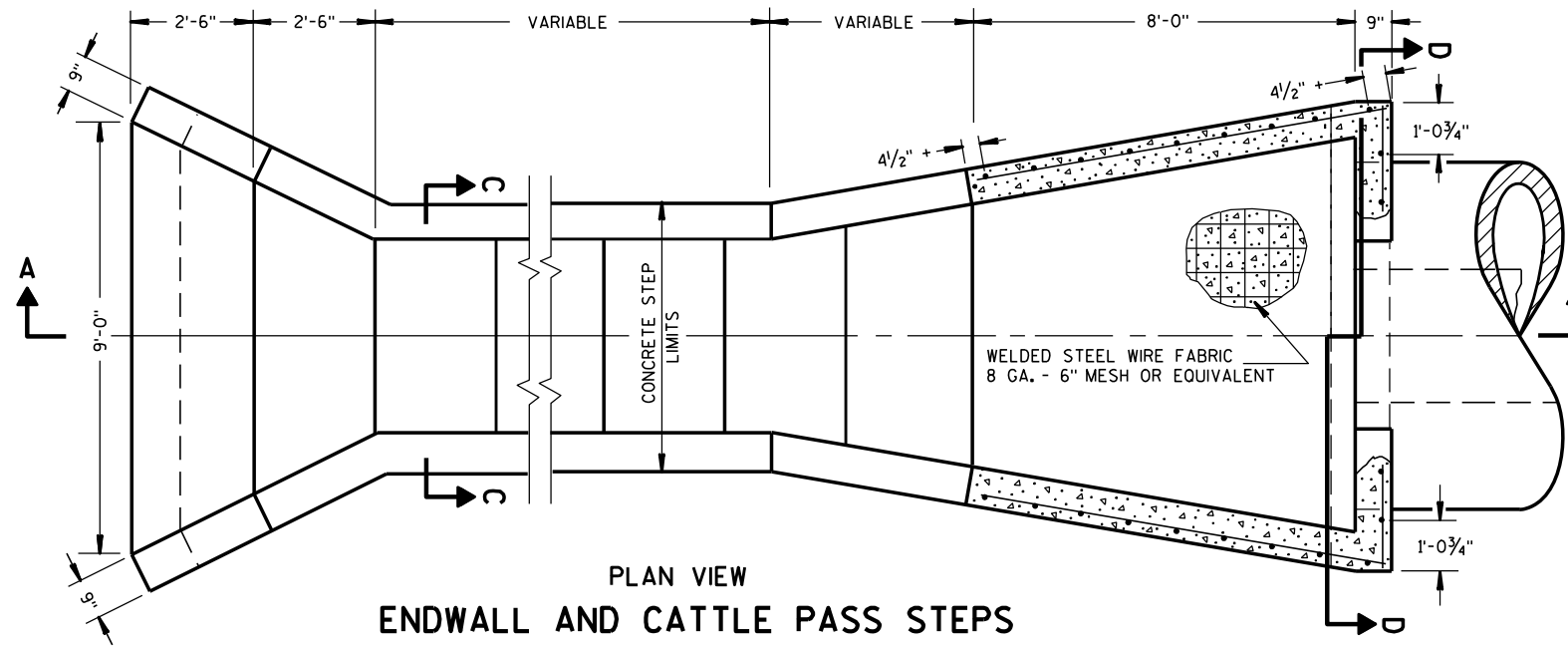
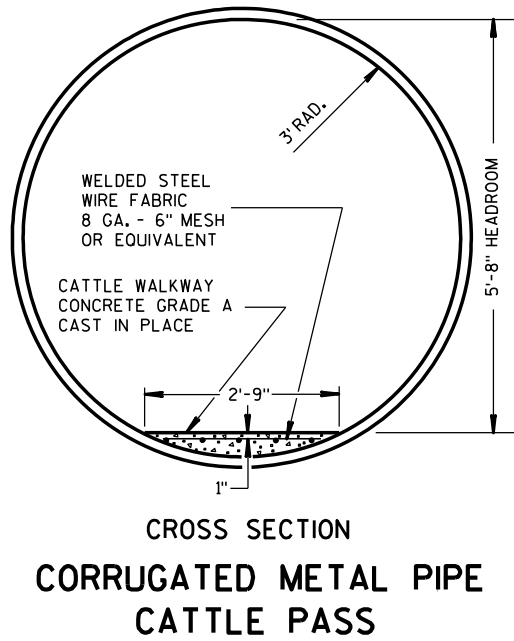
NOTE: DIMPLED BAND FITS OVER OUTSIDE OF ENDWALL, AND CORRUGATED BAND FITS INSIDE ENDWALL.

CONNECTION DETAILS

APRON ENDWALLS FOR PIPE ARCH AND ELLIPTICAL PIPE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
11/30/94 /S/ Rory L. Rhinesmith
DATE CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA



GENERAL NOTES

- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- ALL STEEL REINFORCEMENT IN ENDWALLS AND CATTLE PASS STEPS SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE NOTED.
- ALL STEEL REINFORCEMENT OR FABRIC USED AS SHOWN ABOVE SHALL BE INCIDENTAL TO THE BID ITEM OF WHICH IT IS AN INTEGRAL PART.
- EYE-RODS FOR FENCE CONNECTIONS SHALL BE PROVIDED BY THE CONTRACTOR AS AN INCIDENTAL TO THE BID ITEM OF CONCRETE MASONRY, ENDWALLS AND SHALL BE GALVANIZED.
- CONCRETE USED FOR THE CATTLE WALKWAY WITHIN THE PIPE SHALL BE INCIDENTAL TO THE BID ITEM OF PIPE CATTLE PASS.

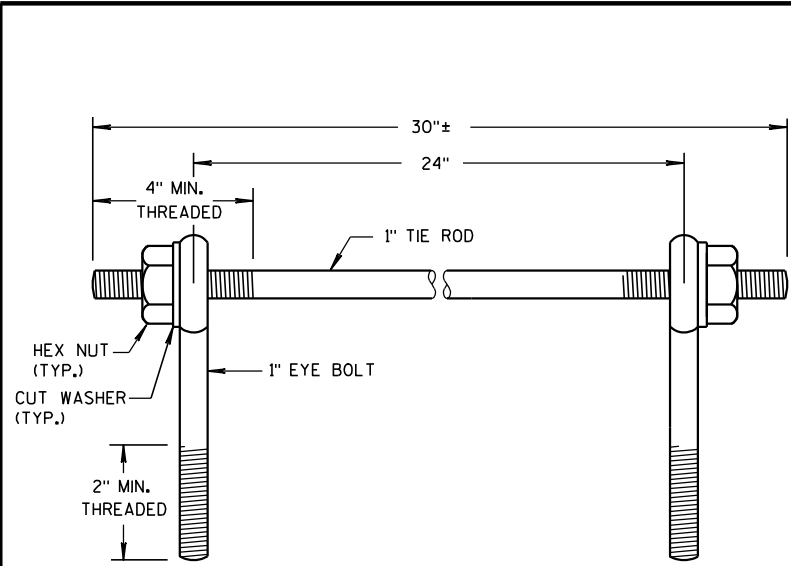
DETAILS FOR PIPE CATTLE PASS, CONCRETE ENDWALL AND STEPS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 6/6/75 DATE	/S/ Harold Fieder STATE DESIGN ENGINEER FOR HWYS
FHWA	

6

6

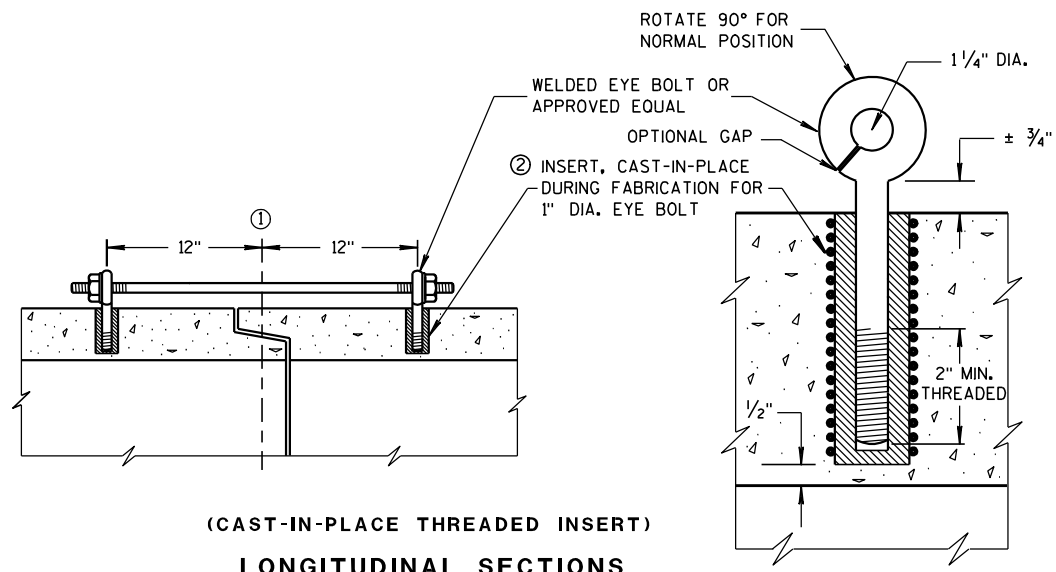
S.D.D. 8 F 3-3

S.D.D. 8 F 3-3



EYE BOLTS AND TIE ROD

EYE BOLT AND TIE ROD ASSEMBLY (ALTERNATE NO. 1)



(CAST-IN-PLACE THREADED INSERT)
LONGITUDINAL SECTIONS

GENERAL NOTES

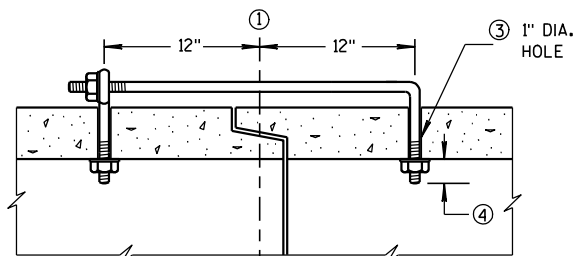
DETAILS OF CONSTRUCTION, MATERIALS, AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

CONCRETE CULVERT AND STORM SEWER PIPE SHALL BE TIED TOGETHER IN THE MANNER ILLUSTRATED BY THIS DETAIL AT LOCATIONS DESIGNATED IN THE STANDARD SPECIFICATIONS AND THE PLAN. THE CONTRACTOR MAY USE EITHER ALTERNATE 1, 2 OR 3 FOR DRAINAGE STRUCTURES. ONLY ALTERNATE 1 AND 3 MAY BE USED FOR CATTLE PASSES, UNLESS OTHERWISE STATED IN THE CONTRACT. THE MATERIALS, FABRICATION AND WORK NECESSARY TO TIE THE PIPE BY THIS DETAIL WILL BE CONSIDERED INCIDENTAL TO THE PIPE AND APRON ENDWALLS IF REQUIRED.

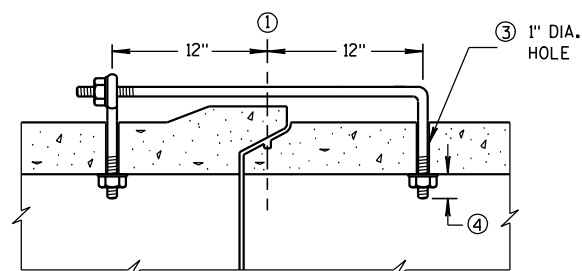
DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR JOINT TIES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.

JOINT TIES TO BE HOT-DIP GALVANIZED PER ASTM A 153.

- ① ϕ OF TONGUE AND GROOVE OR BELL AND SPIGOT JOINTS.
- ② THE INSIDE OF THE THREADED INSERTS SHALL BE CLEAN TO ALLOW THE INSERTION OF THREADED EYE BOLTS.
- ③ HOLES SHALL BE CAST-IN-PLACE OR DRILLED 12 INCHES FROM ϕ OF TONGUE AND GROOVE.
- ④ BOLT PROJECTION INSIDE OF PIPE SHALL NOT EXCEED 2 INCHES.
- ⑤ OPENING TO BE ROD DIAMETER PLUS 1 INCH.
- ⑥ LENGTH ADEQUATE TO EXTEND TO WITHIN $\frac{1}{2}$ INCH OF THE INNER SURFACE OF THE PIPE.



(TONGUE & GROOVE PIPE)



(MODIFIED BELL PIPE)
LONGITUDINAL SECTION

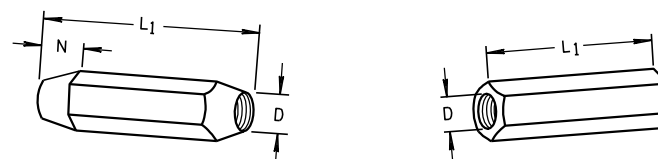
EYE BOLT DIMENSION TABLE

PIPE SIZE	L = LENGTH	
	TONGUE & GROOVE PIPE	MODIFIED BELL PIPE
18" TO 24"	4 1/2"	6 1/4"
30"	5"	7"
36"	5 1/2"	7"
42"	6"	
48"	6 1/2"	
60"	7 1/2"	
66"	8"	

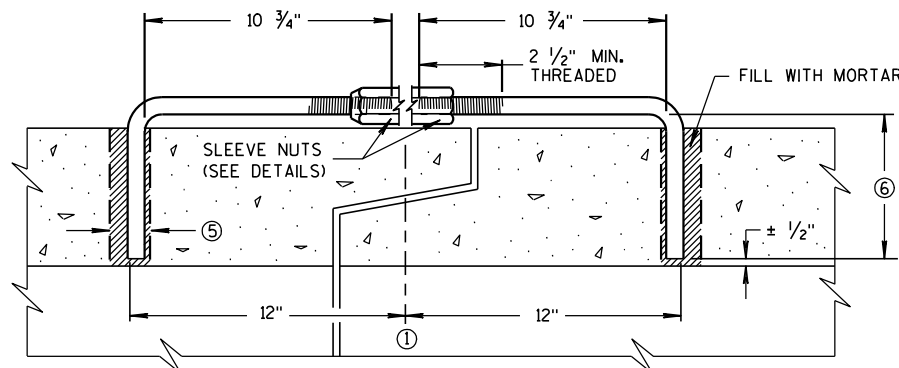
ADJUSTABLE TIE ROD TABLE

PIPE DIAMETER	TIE ROD DIAMETER	D	L ₁	N
12-60	5/8	5/8	5	1/2
66-84	3/4	3/4	5	1/2
90-108	1	1	7	1 1/6

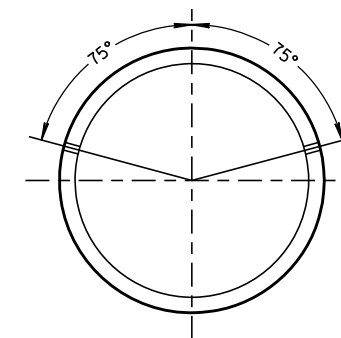
DIMENSIONS SHOWN ARE IN INCHES



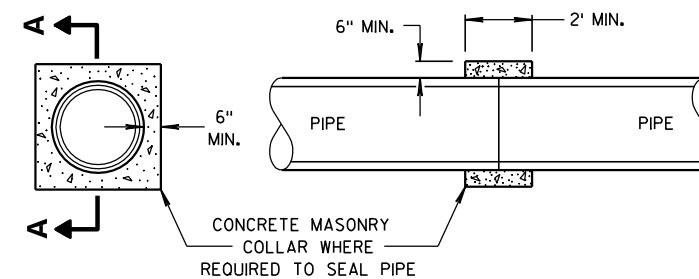
TAPERED PLAIN
RIGHT AND LEFT THREADS
SLEEVE NUTS



(JOINT TIES FOR 12" TO 108" DIA. CONCRETE PIPE)
LONGITUDINAL SECTION
ADJUSTABLE TIE ROD (ALTERNATE NO. 3)



PLACEMENT OF (2) CAST-IN-PLACE
INSERTS OR HOLES DURING FABRICATION
FOR PIPE SECTIONS REQUIRING TIE RODS
TRANSVERSE SECTION



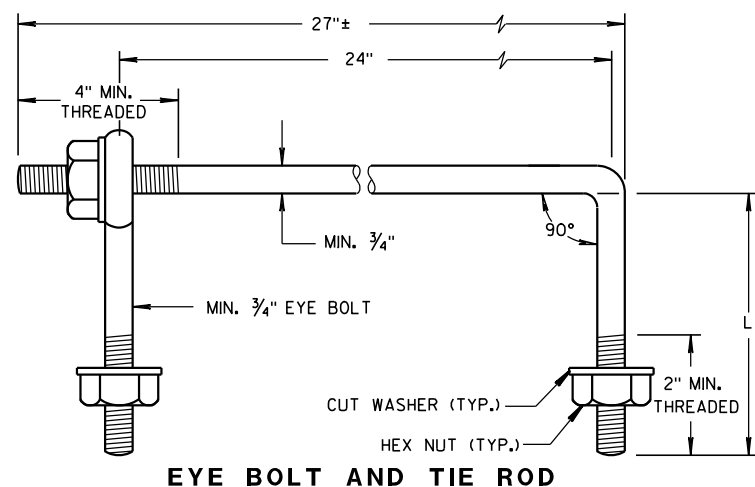
SECTION A-A
CONCRETE COLLAR DETAIL

JOINT TIES FOR CONCRETE
PIPE AND CONCRETE
COLLAR DETAIL

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
6/5/2012 /S/ Jerry H. Zogg
DATE ROADWAY STANDARDS DEVELOPMENT
ENGINEER
FHWA

(JOINT TIES FOR 18" TO 66" DIA. CONCRETE PIPE)
EYE BOLT AND TIE ROD ASSEMBLY (ALTERNATE NO. 2)

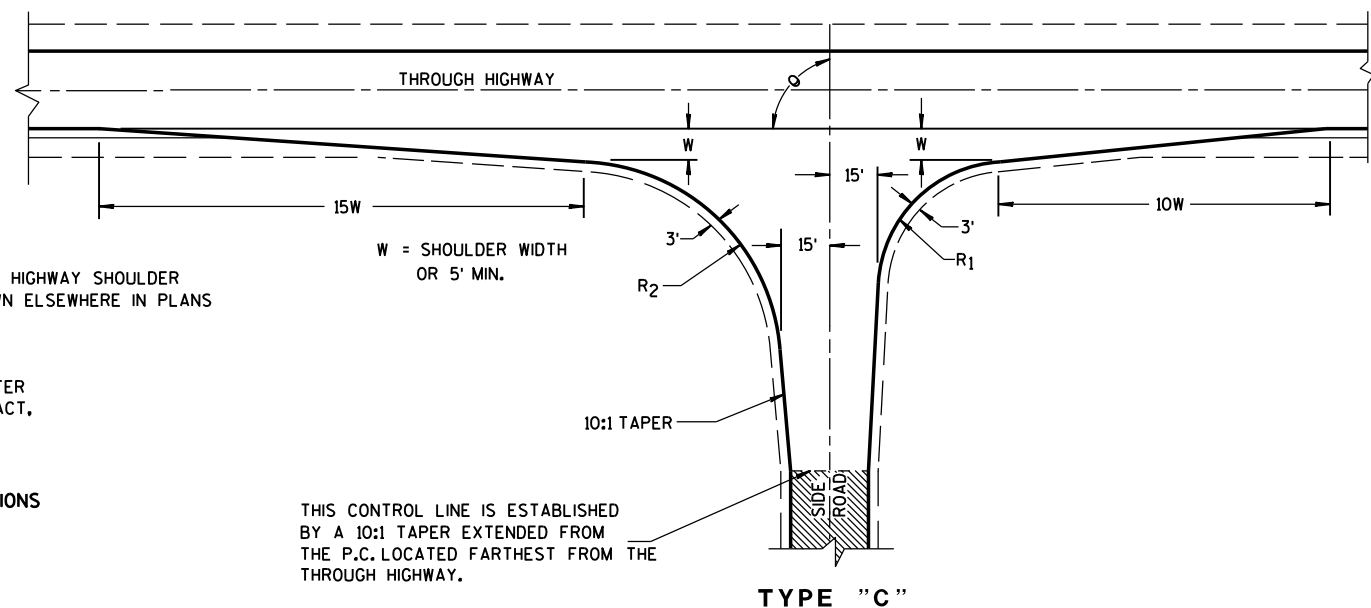
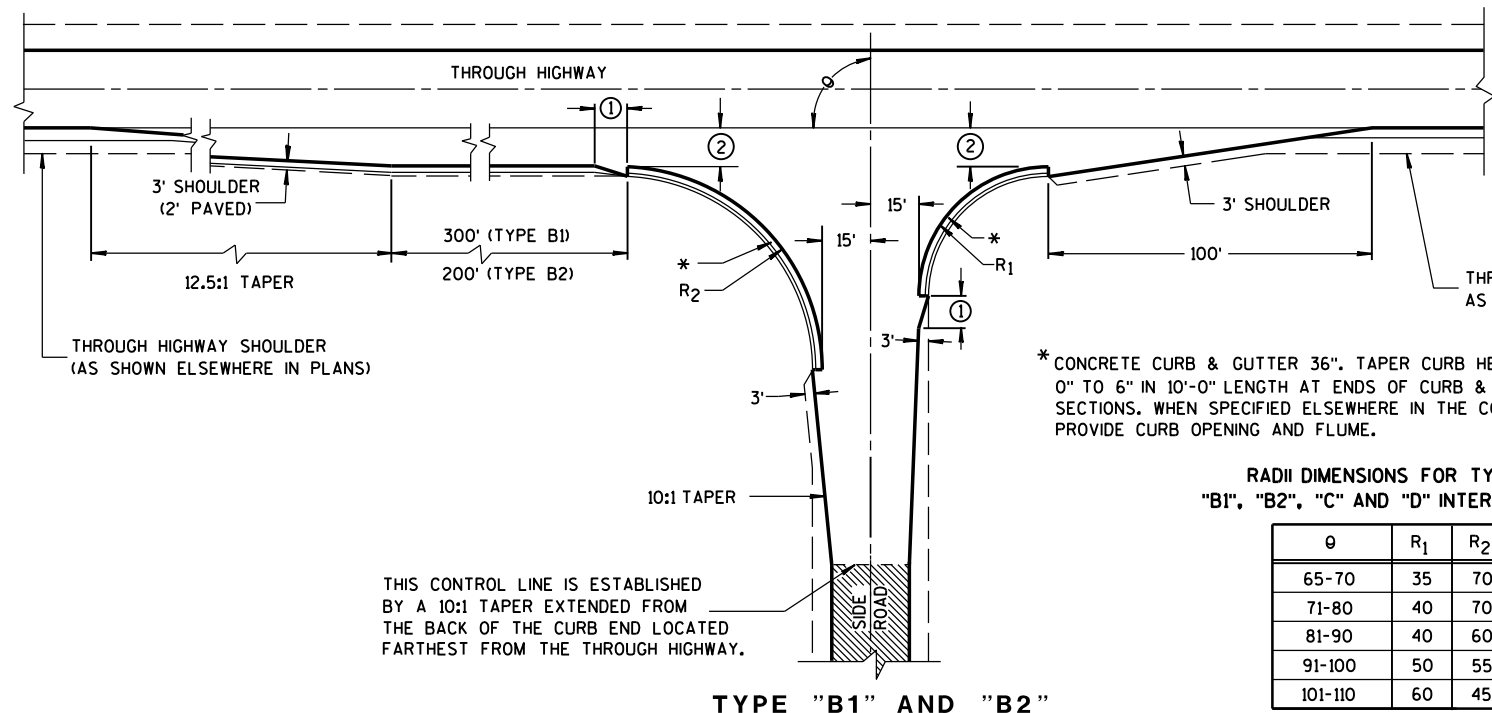


EYE BOLT AND TIE ROD

EYE BOLT

NOTE: TWO EYE BOLTS MAY BE USED WITH
A 30" LONG THREADED ROD IN LIEU
OF THE 90° BENT TIE ROD.

EYE BOLT AND TIE ROD ASSEMBLY (ALTERNATE NO. 2)



RADII DIMENSIONS FOR TYPES "B1", "B2", "C" AND "D" INTERSECTIONS

θ	R ₁	R ₂
65-70	35	70
71-80	40	70
81-90	40	60
91-100	50	55
101-110	60	45

GENERAL NOTES

DESIGNS MAY BE USED INTERCHANGEABLY IN COMBINATION OR SEPARATELY FOR ANY ONE COMPLETE INTERSECTION DEPENDING UPON INTERSECTION ANGLE AND SURFACING OF EACH APPROACH ROADWAY.

SIDE ROAD SURFACING NOTE

WHEN THE SIDE ROAD IS NOT PRESENTLY PAVED, PAVEMENT SHALL BE PLACED TO THE LIMITS SHOWN UNLESS OTHERWISE PROVIDED IN THE CONTRACT. WHERE THE CONSTRUCTION LIMITS ARE BEYOND THE PAVING LIMITS, CRUSHED AGGREGATE SURFACING SHALL BE PLACED BETWEEN THE PAVING LIMITS AND CONSTRUCTION LIMITS.

WHEN THE SIDE ROAD IS PRESENTLY PAVED, NEW PAVEMENT SHALL BE PLACED TO THE LIMITS OF DESIGN AS SHOWN AND BEYOND, IF NECESSARY, TO MEET EXISTING PAVEMENT.

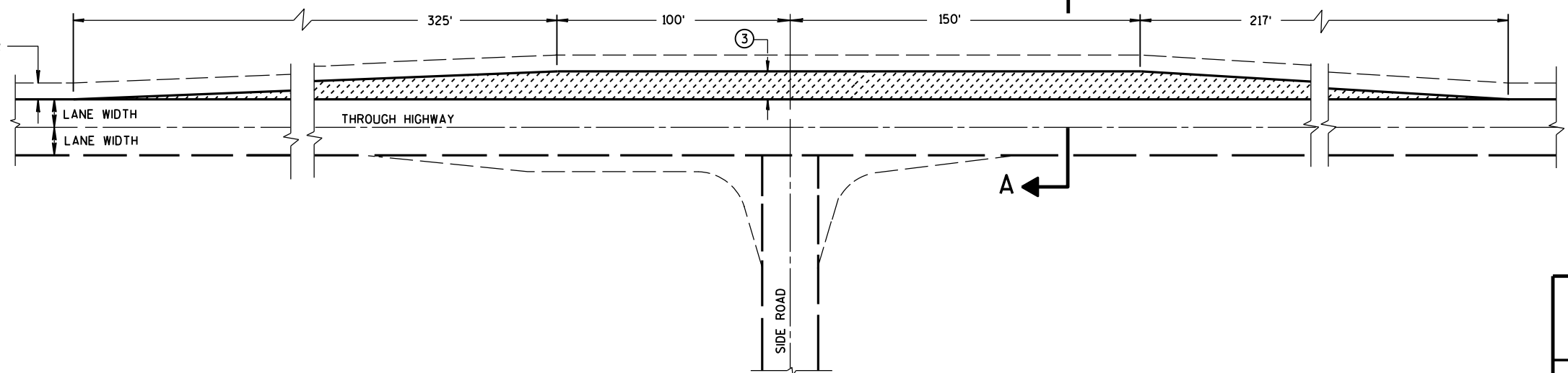
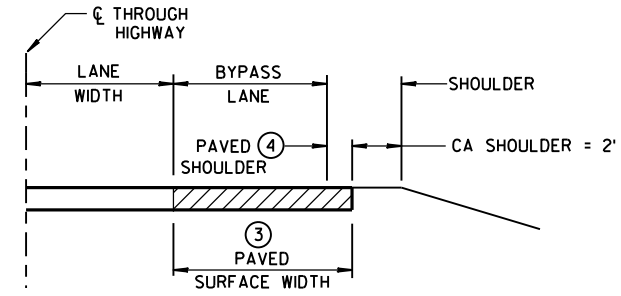
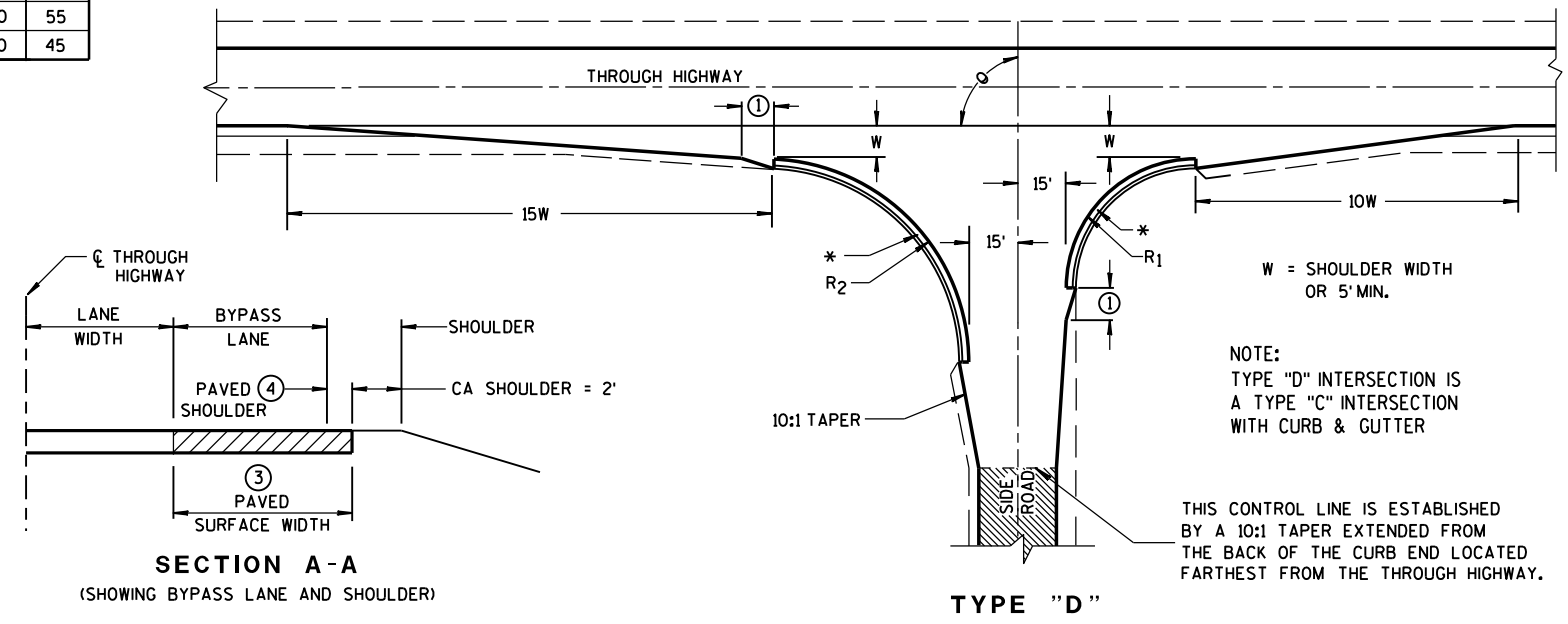
WHEN THE SIDE ROAD IS THE CONSTRUCTION PROJECT, THE INTERSECTION SURFACING SHALL BE THE SAME AS FOR THE PROJECT.

EXISTING PAVED SURFACE

BYPASS LANE

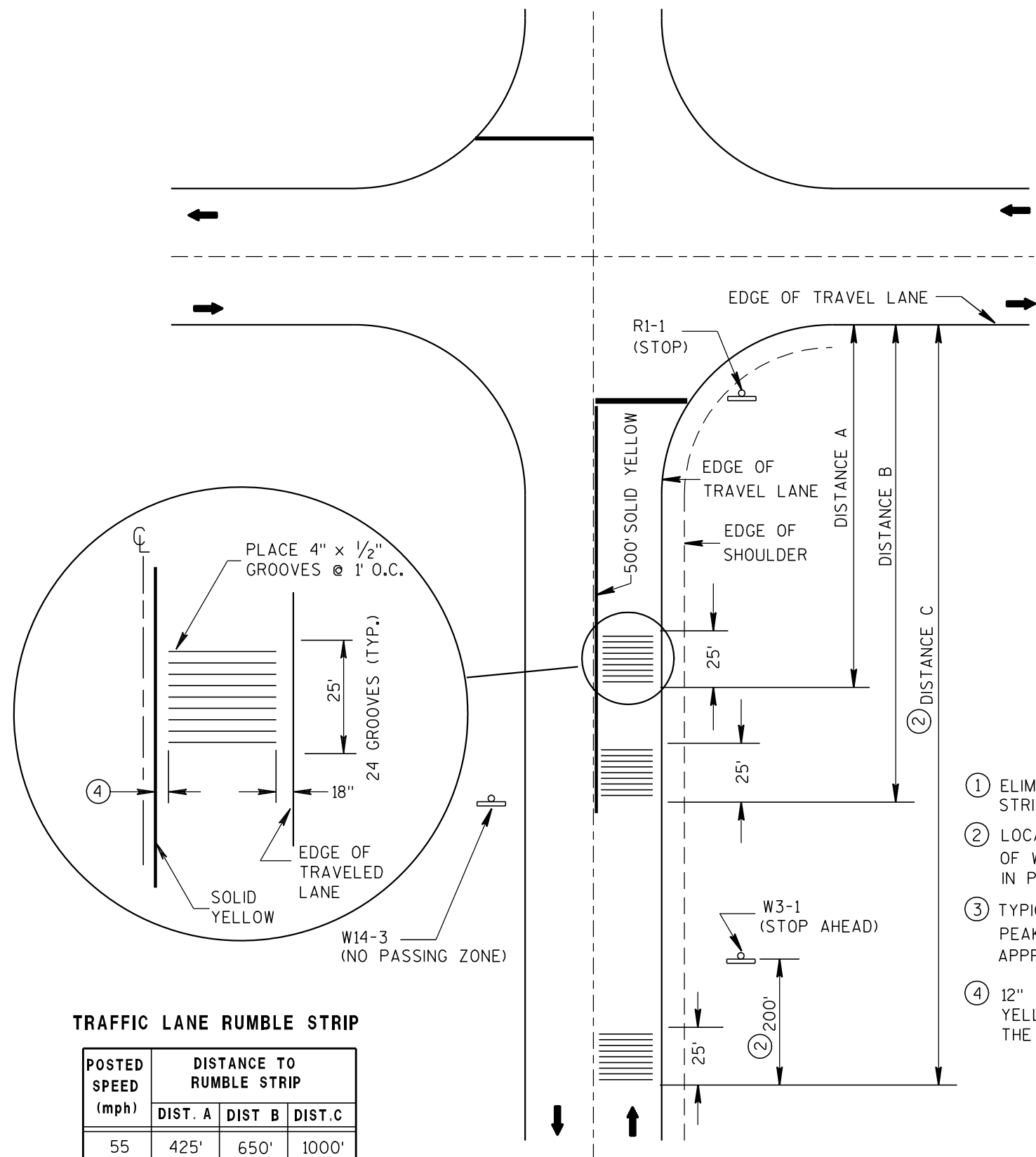
- ① 10-FT TYPICAL.
- ② 12-FT** PLUS ADDITIONAL WIDTH FOR BIKE LANE IF SHOWN ELSEWHERE IN THE PLAN.

**10-FT MAY BE USED ON TYPE B2 ON RESURFACING PROJECTS IF SPECIFIED IN THE CONTRACT.
- ③ BYPASS LANE PAVED SURFACE WIDTH OUTSIDE OF TRAVEL LANE
-ASPHALT = 12-FT PLUS PAVED SHOULDER WIDTH.
-PC CPNCRETE = 13-FT PLUS PAVED SHOULDER WIDTH.
- ④ BYPASS LANE PAVED SHOULDER WIDTH = THE GREATER OF 1-FT OR THE PAVED SHOULDER WIDTH OF THE THROUGH HIGHWAY.



TEE INTERSECTION BYPASS LANE DETAIL

AT-GRADE SIDE ROAD INTERSECTION, TYPES "B1", "B2", "C" AND "D" AND TEE INTERSECTION BYPASS LANE
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION



TRAFFIC LANE RUMBLE STRIP

POSTED SPEED (mph)	DISTANCE TO RUMBLE STRIP		
	DIST. A	DIST. B	DIST. C
55	425'	650'	1000'
50	325'	450'	800'
45	275'	400'	650'
40	225'	①	550'
35	175'	①	475'
≤ 30	125'	①	425'

ARROW SYMBOL (➔) SHOWS DIRECTION OF TRAVEL

**PLAN VIEW
RUMBLE STRIP LOCATION**

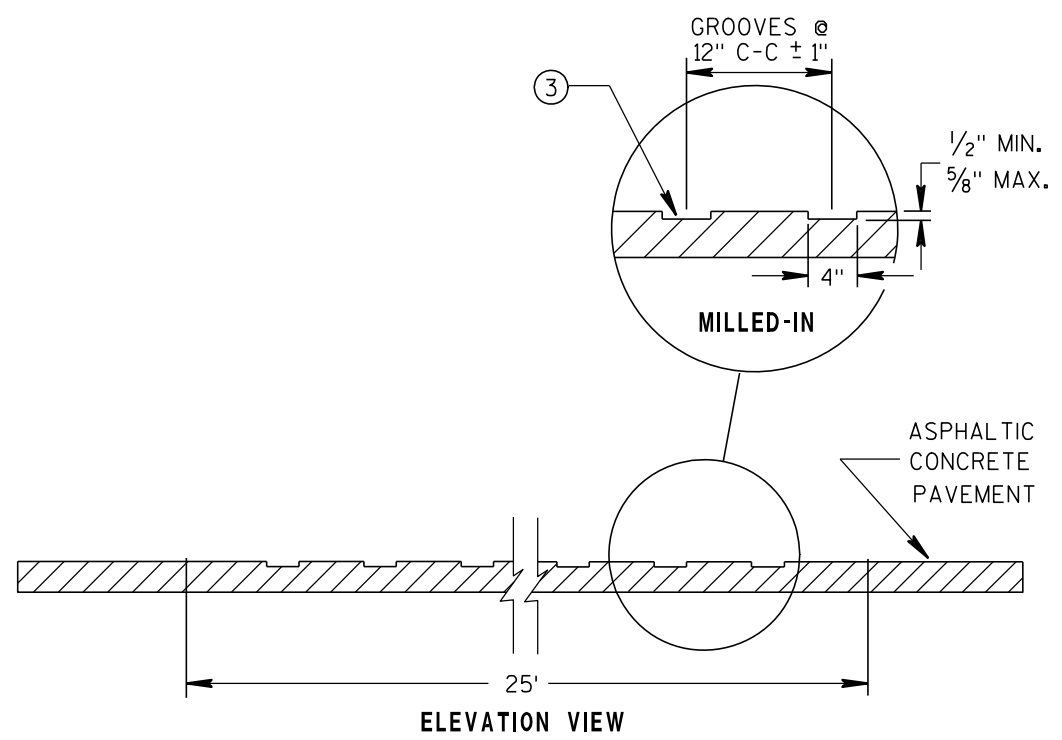
- ① ELIMINATE THE MIDDLE SET OF RUMBLE STRIPS.
- ② LOCATE RUMBLE STRIP 200' IN ADVANCE OF W3-1 SIGN AS SHOWN. IF W3-1 IS NOT IN PLACE, USE DISTANCE C.
- ③ TYPICAL VERTICAL VARIATION BETWEEN PEAKS AND VALLEYS WITHIN THE CUT APPROXIMATELY 1/16"
- ④ 12" CLEAR BETWEEN THE SOLID YELLOW LINE AND THE EDGE OF THE RUMBLE.

GENERAL NOTES

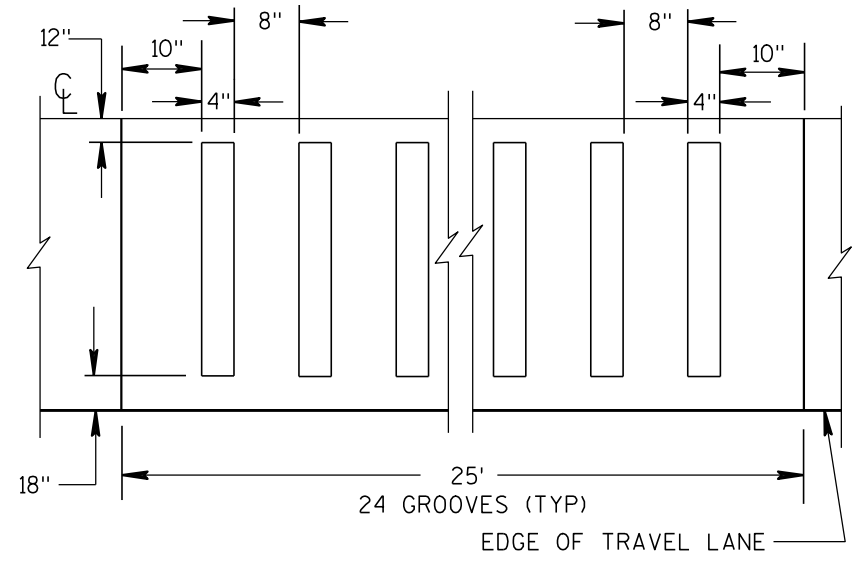
CONTRACTOR SHALL CONFIRM RUMBLE STRIP LOCATION WITH THE ENGINEER PRIOR TO INSTALLATION. THE ENGINEER MAY MODIFY THE RUMBLE STRIP LOCATION AS FIELD CONDITIONS DICTATE.

WHEN ASPHALTIC PAVEMENT IS NEW IN THE RUMBLE AREA THE CONTRACTOR SHALL ALLOW THE PAVEMENT TO CURE A MINIMUM OF 7 DAYS PRIOR TO RUMBLE INSTALLATION.

PAVEMENT MARKING AND SIGNING DETAILS AND SPECIFICATIONS ARE PROVIDED ELSEWHERE IN THE CONTRACT.

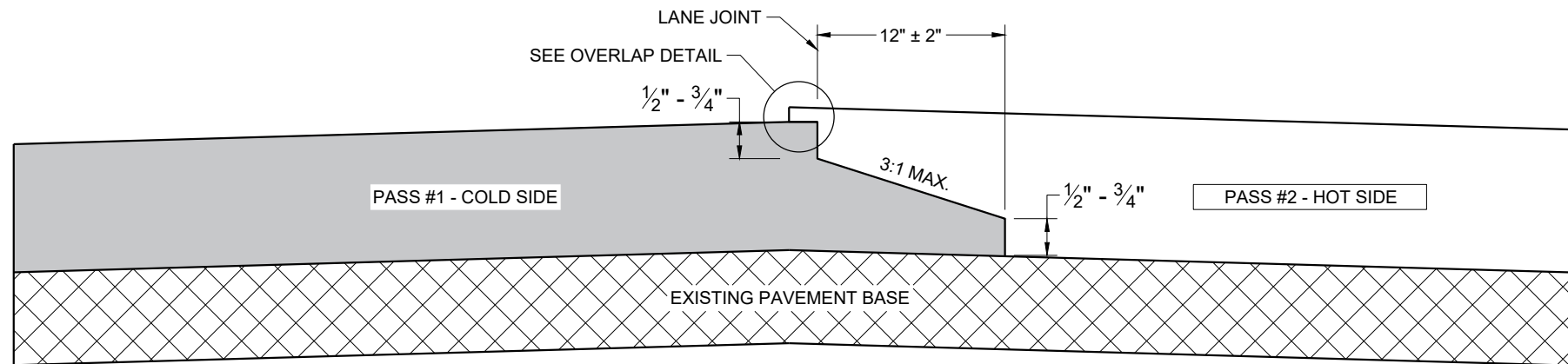


ELEVATION VIEW

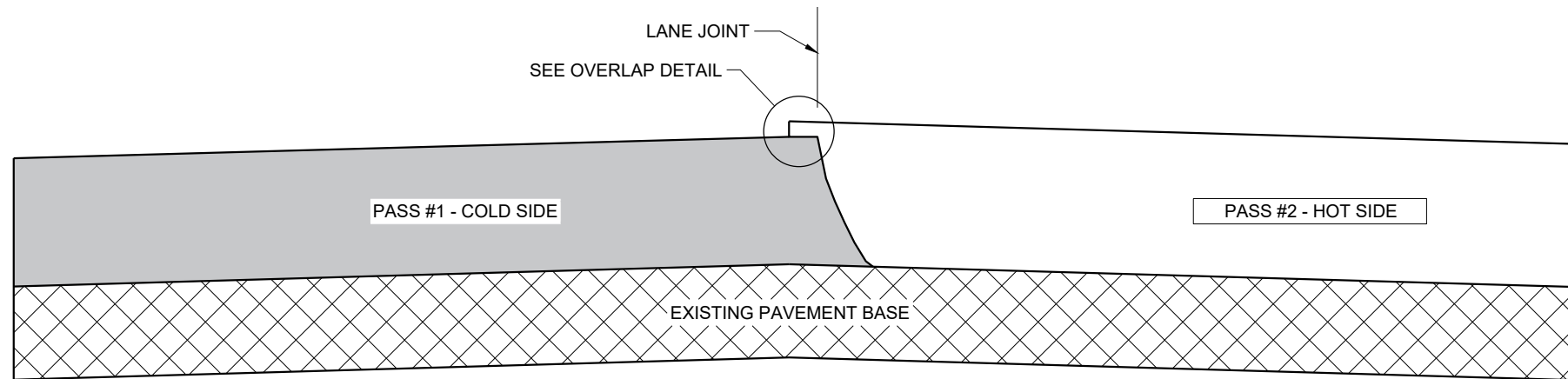


**PLAN VIEW
ASPHALTIC PAVEMENT
MILLED-IN**

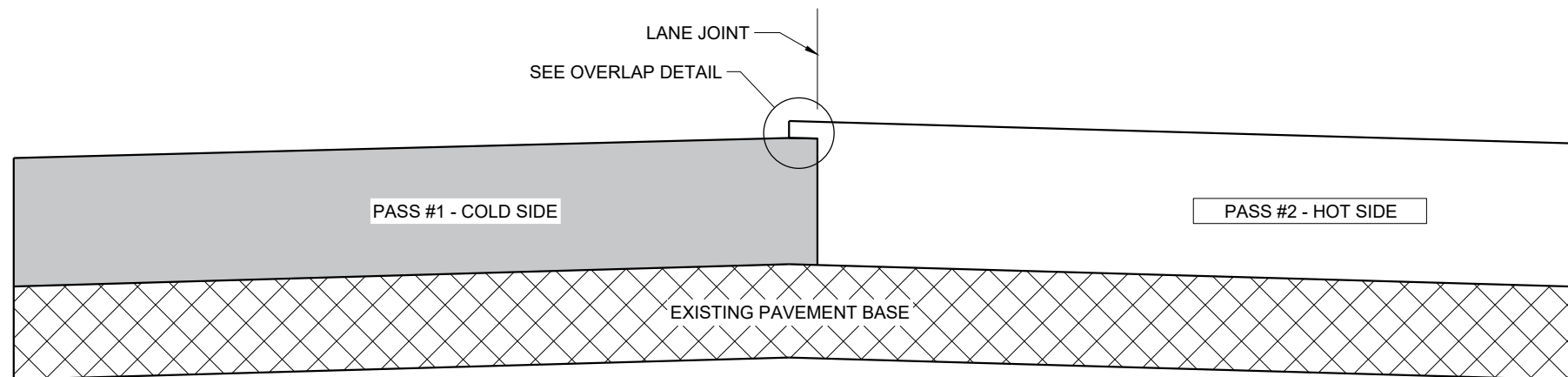
ASPHALTIC RUMBLE STRIPS AT INTERSECTION	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 8/17/2011 DATE	/S/ Jerry H. Zogg ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA	



**TYPICAL PAVEMENT CROSS SECTION
NOTCHED WEDGE JOINT**



**TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT**



**TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT (MILLED)**

GENERAL NOTES

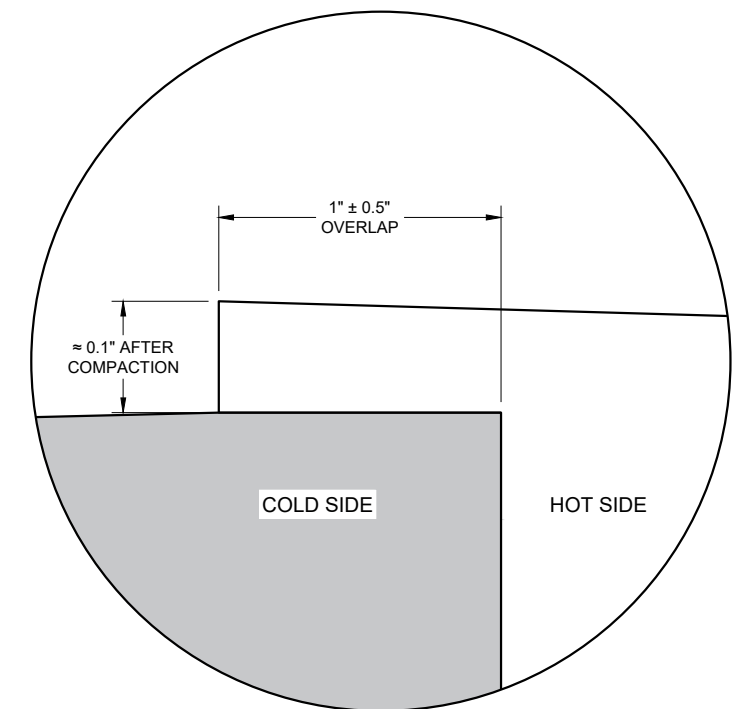
IN ADDITION TO THE DETAILS PROVIDED IN THIS DRAWING, CONFORM TO STANDARD SPECIFICATION 450.3.2.8 FOR WHEN A NOTCHED WEDGE JOINT IS REQUIRED AND FOR GENERAL JOINT CONSTRUCTION REQUIREMENTS.

FOR ALL LONGITUDINAL JOINTS, ENSURE THE PAVER SCREED OVERLAPS THE PREVIOUSLY PLACED PAVEMENT BY $1" \pm 0.5"$ AND THE HOT SIDE OF THE JOINT REMAINS HIGHER THAN THE COLD SIDE BY APPROXIMATELY 0.1" AFTER FINAL COMPACTION. (IT WILL BE FLUSH WHEN PAVING IN ECHELON.)

ONLY REMOVE THE LONGITUDINAL NOTCHED WEDGE JOINT FOR SMA PAVEMENT OR AS DIRECTED BY THE ENGINEER TO ADDRESS SPECIFIC LENGTHS OF JOINT DAMAGED BY TRAFFIC.

WHEN MILLING BACK OR REMOVING ANY LONGITUDINAL JOINT, LIMIT THE MATERIAL REMOVED TO 2" FROM THE TOP NOTCH OR FROM THE VERTICAL JOINT EDGE ON THE COLD SIDE OF THE JOINT.

USE LONGITUDINAL MILLED JOINT AS PLANS SHOW OR THE AS THE ENGINEER DIRECTS.



OVERLAP DETAIL (TYPICAL)

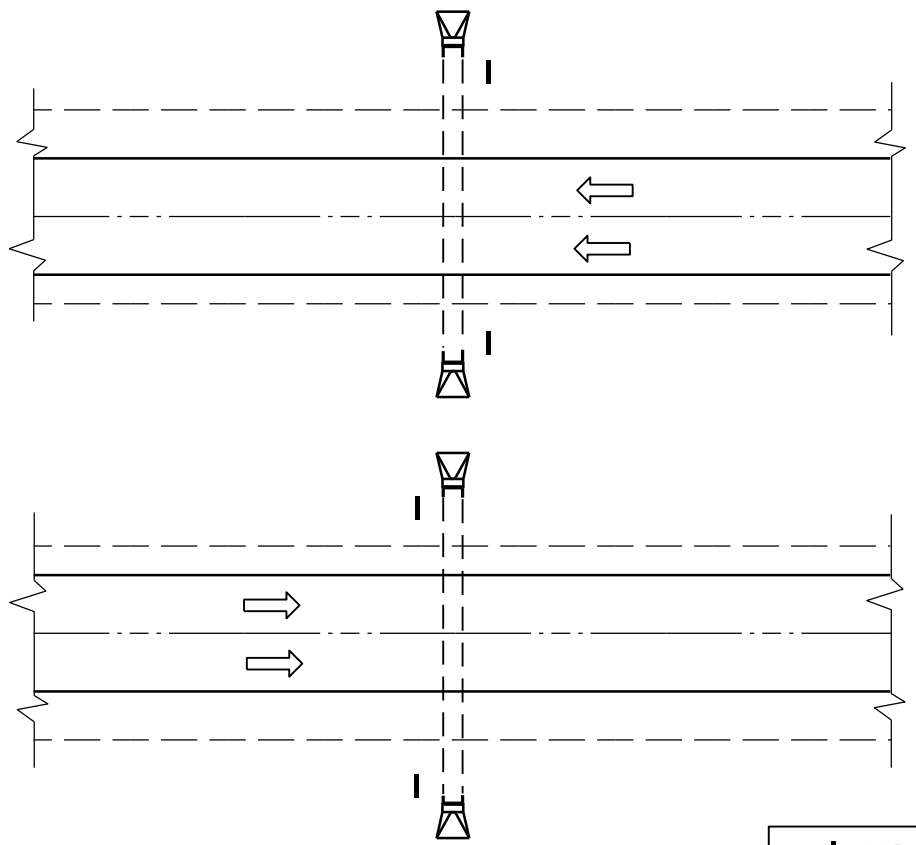
6

6

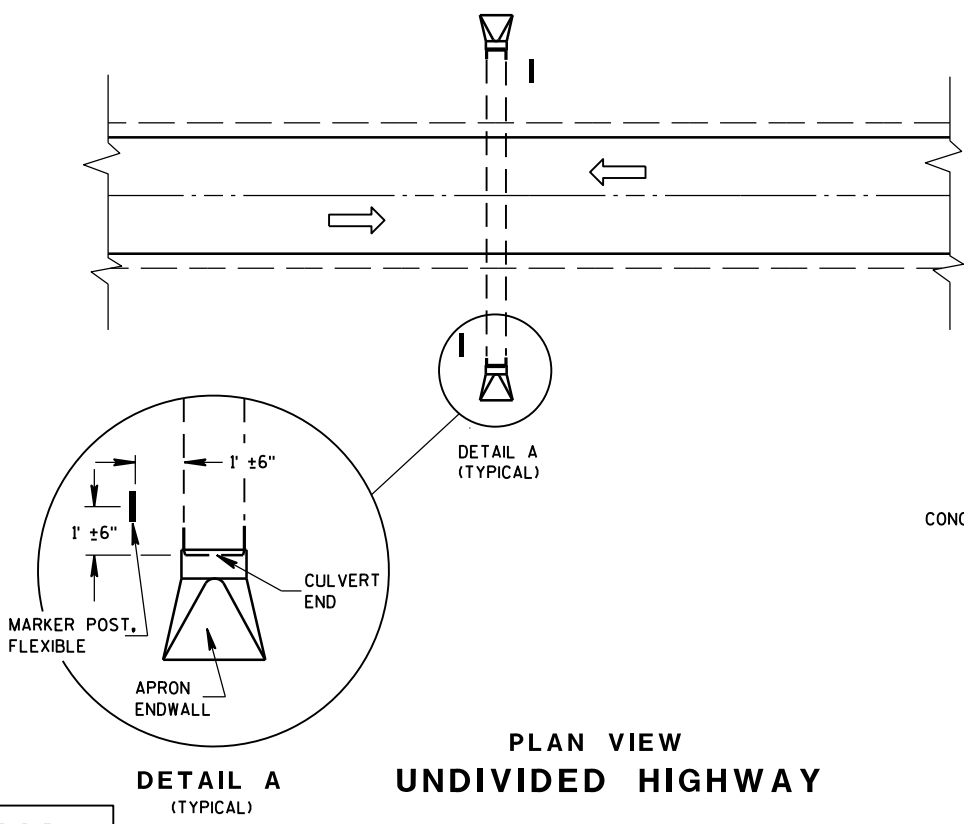
SDD 13C19 - 03

SDD 13C19 - 03

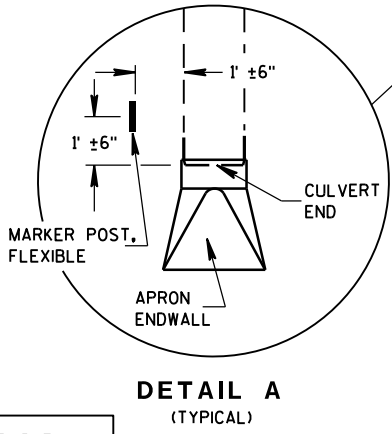
HMA LONGITUDINAL JOINTS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED November 2020 DATE	/S/ Steven Hefel HMA PAVEMENT ENGINEER
FHWA	



PLAN VIEW
DIVIDED HIGHWAY



PLAN VIEW
UNDIVIDED HIGHWAY

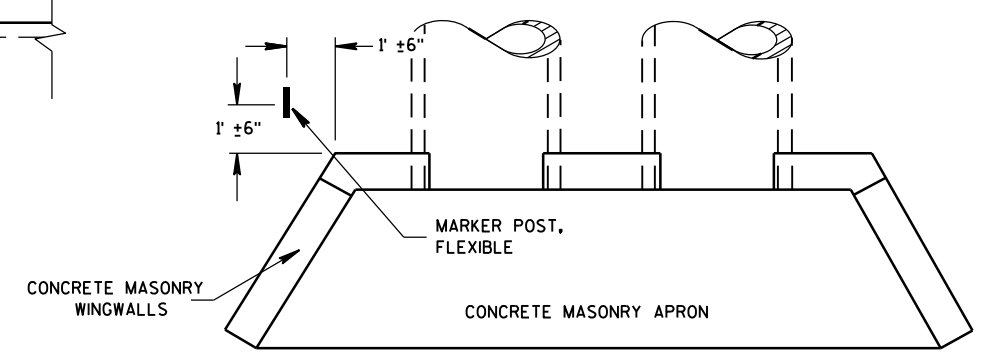


MARKER POST, FLEXIBLE
DIRECTION OF TRAFFIC FLOW

FLEXIBLE MARKER POST LOCATION

GENERAL NOTES

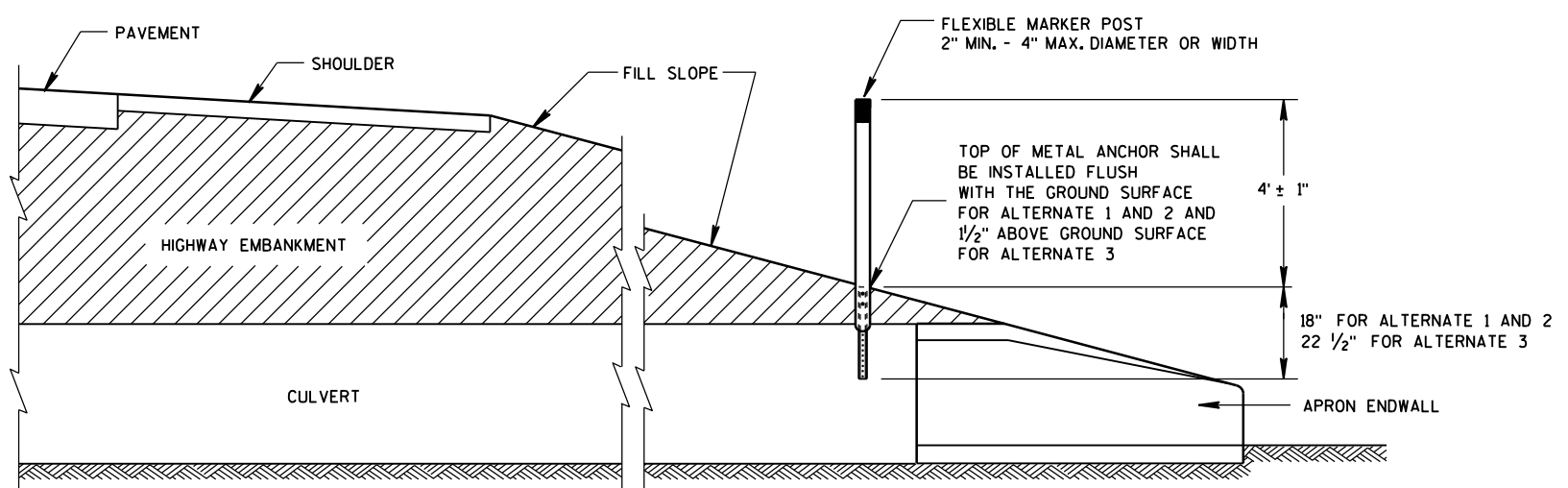
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.



PLAN VIEW
CONCRETE MASONRY ENDWALLS FOR
CULVERT PIPE AND PIPE ARCH

6

6

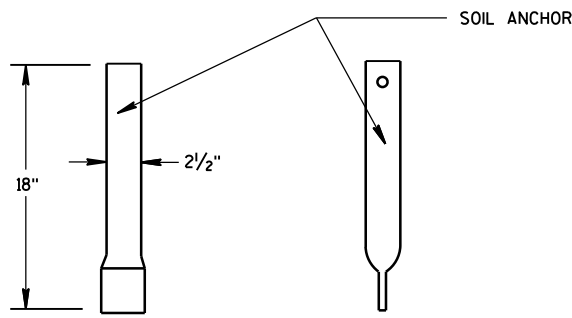
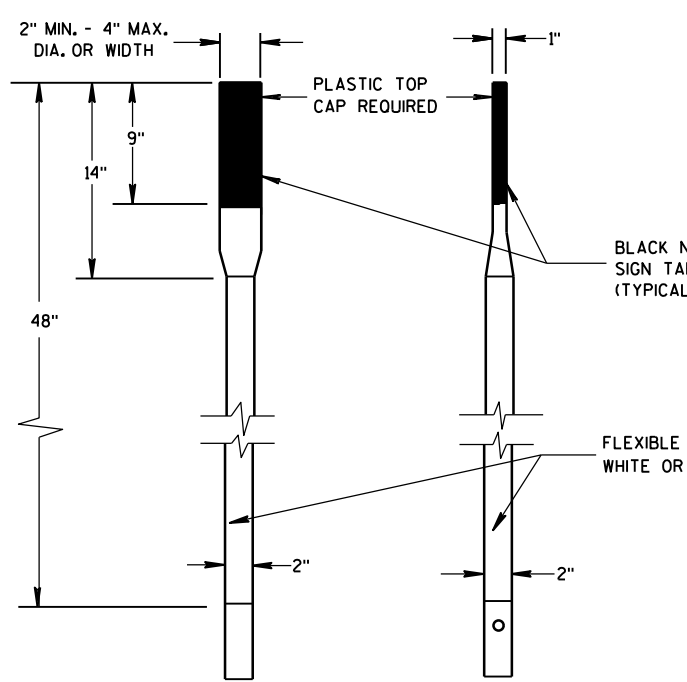


CROSS SECTION
FLEXIBLE MARKER POST

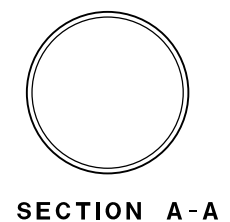
FLEXIBLE MARKER POST
FOR CULVERT END
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

S.D.D. 15 A 3-2a

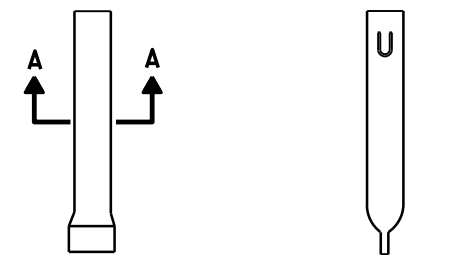
S.D.D. 15 A 3-2a



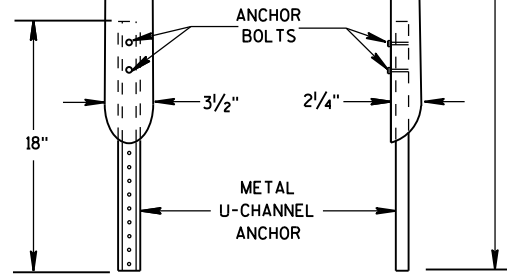
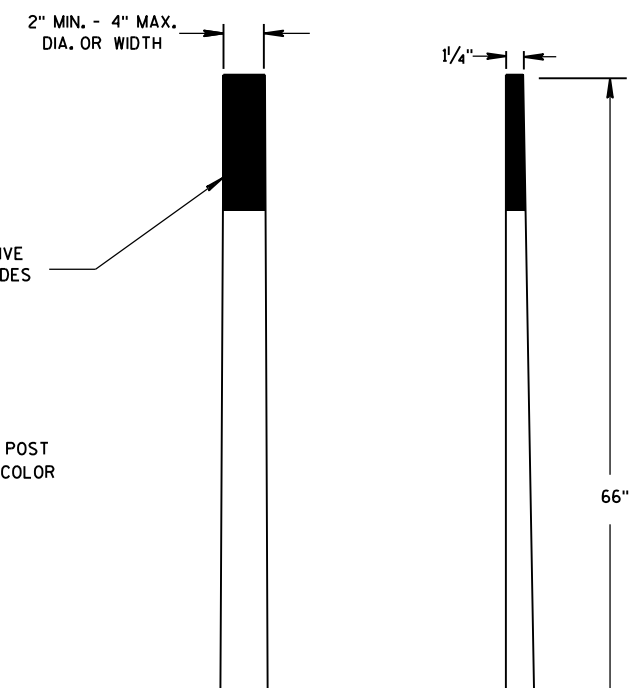
FRONT VIEW SIDE VIEW
ALTERNATE 1



SECTION A-A

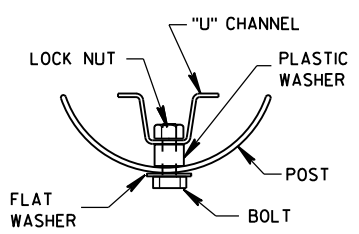


FRONT VIEW SIDE VIEW
ALTERNATE 1

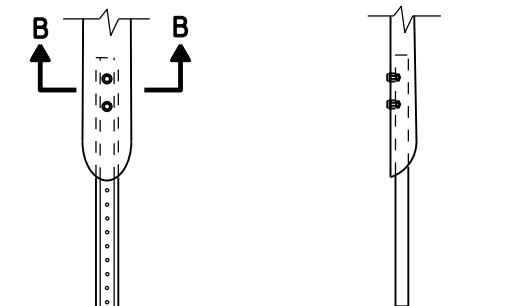


FRONT VIEW SIDE VIEW
ALTERNATE 2

FLEXIBLE MARKER POSTS

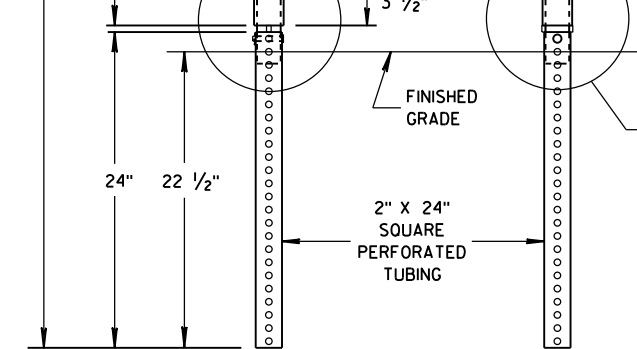
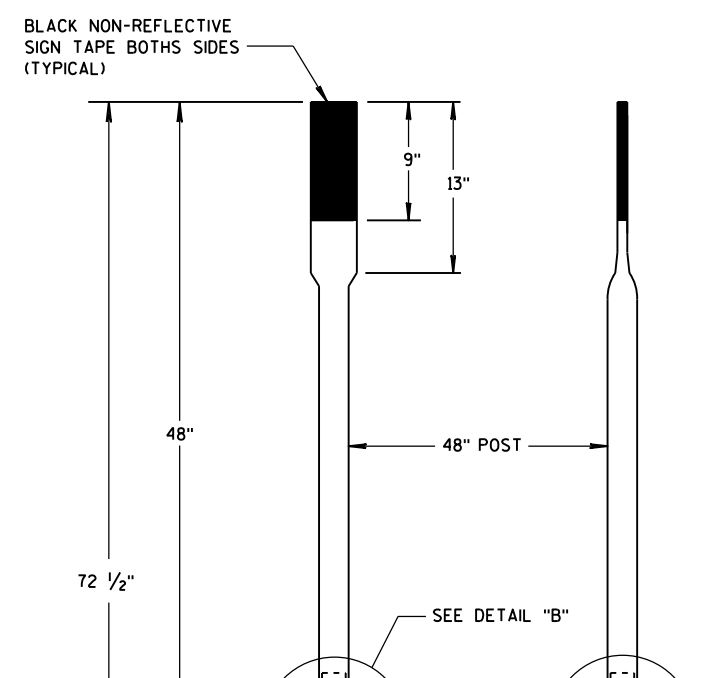


SECTION B-B

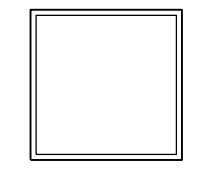


FRONT VIEW SIDE VIEW
ALTERNATE 2

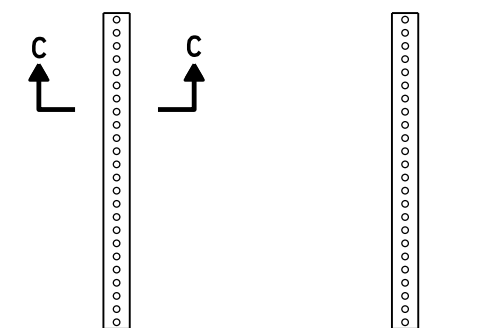
FLEXIBLE MARKER POST ANCHORS



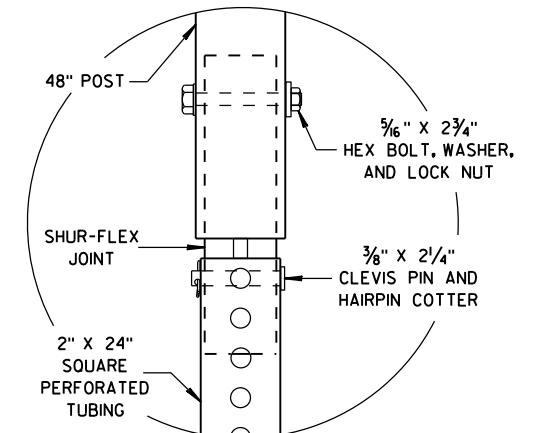
FRONT VIEW SIDE VIEW
ALTERNATE 3



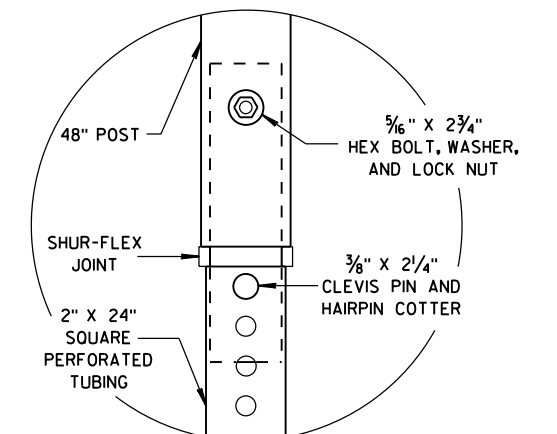
SECTION C-C



FRONT VIEW SIDE VIEW
ALTERNATE 3



DETAIL B

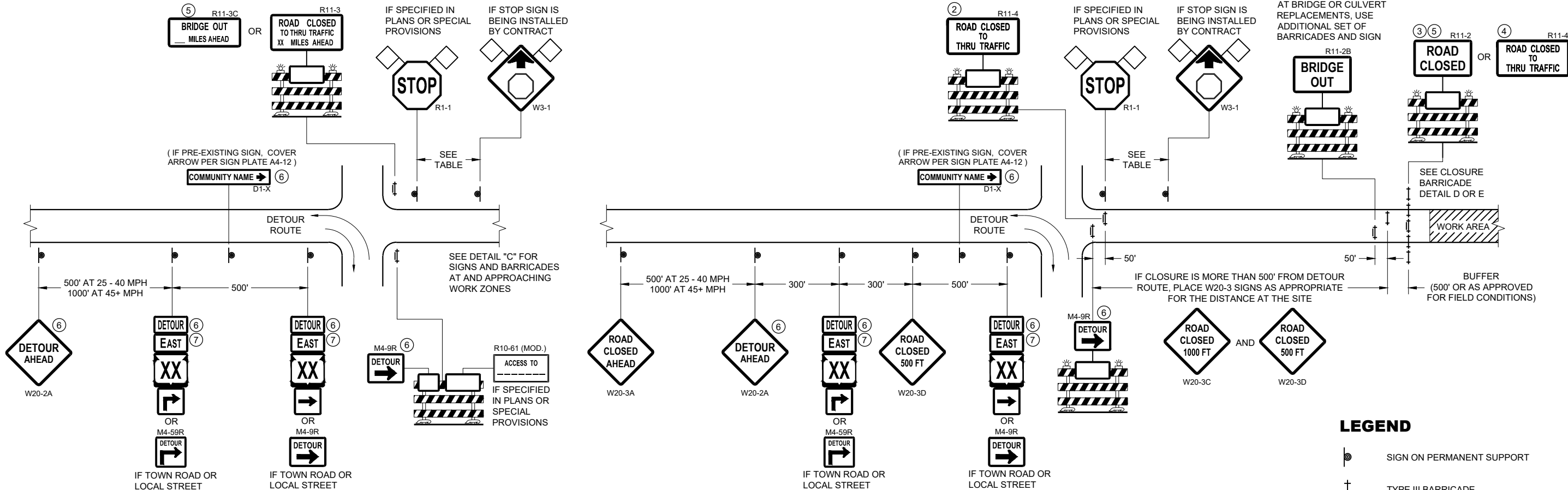


DETAIL C

FLEXIBLE MARKER POST FOR CULVERT END

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
10/1/2012 DATE /S/ Travis Feltes
STATE TRAFFIC ENGINEER OF DESIGN
FHWA



**DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR**

WORK ZONE GREATER THAN OR EQUAL TO 1/2 MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)

**DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR**

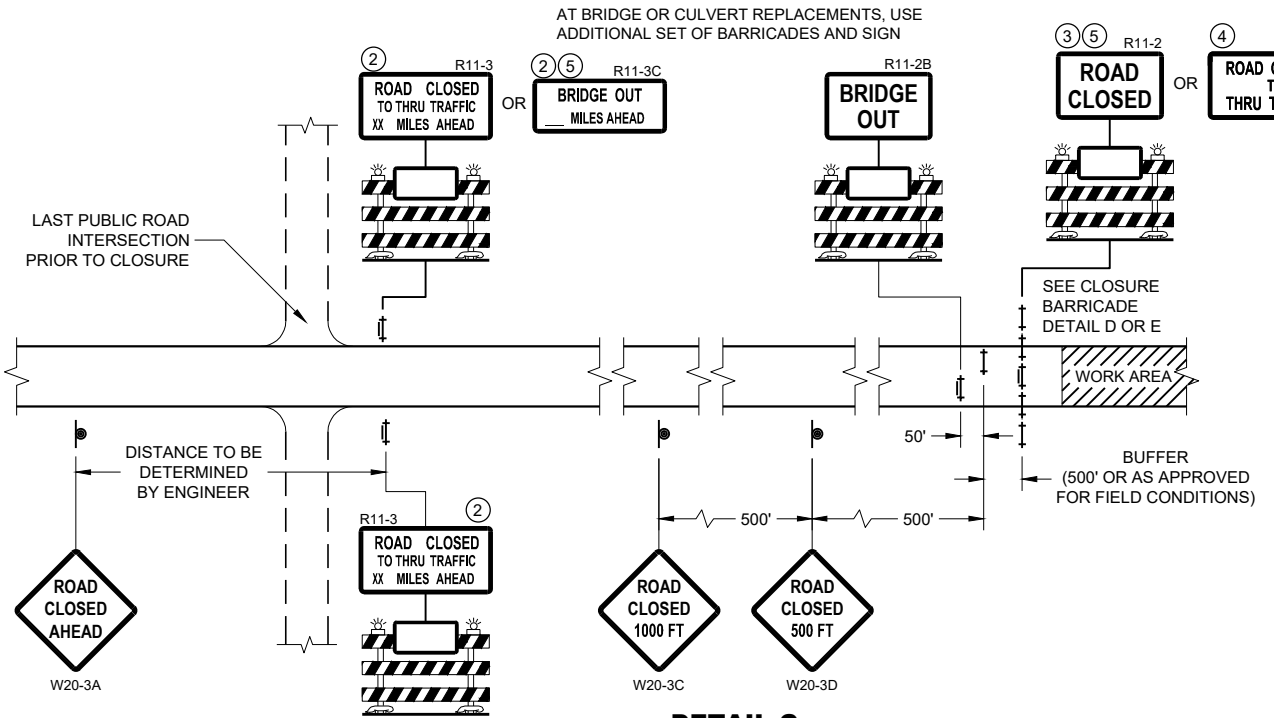
WORK ZONE LESS THAN 1/2 MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
- FLAGS, 16" X 16" MIN. (ORANGE)

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

- M4 - 8
- M3 - X
- OR OR M1 - 4 M1 - 6 M1 - 5A
- OR M05 - 1 M06 - 1



**DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR**

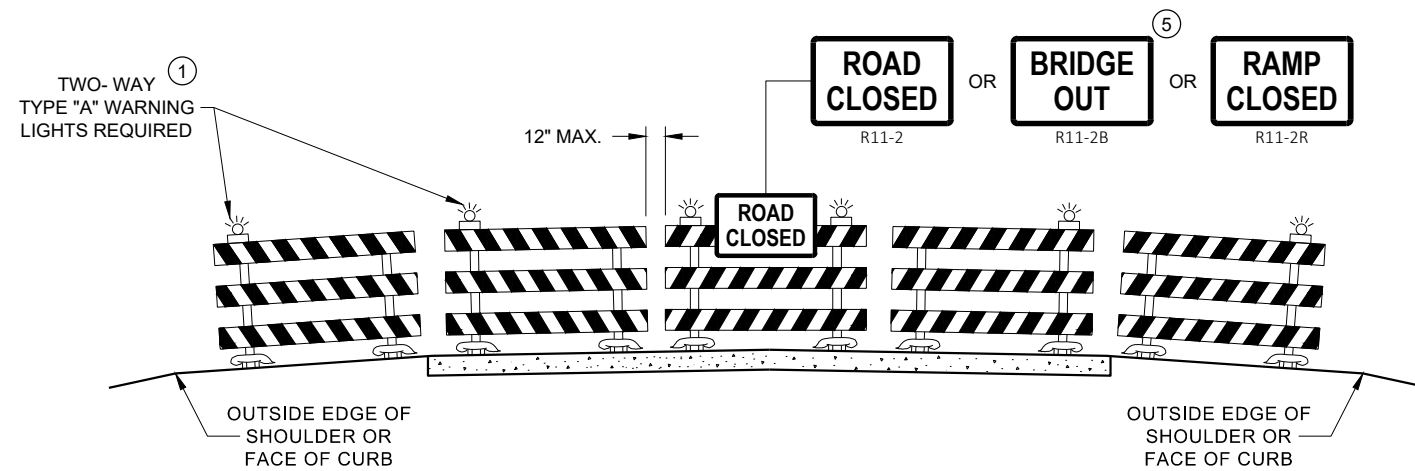
SEE SDD 15C2-SHEET "b"
FOR GENERAL NOTES
AND FOOTNOTES ① THROUGH ⑦

**BARRICADES AND SIGNS
FOR MAINLINE CLOSURES**

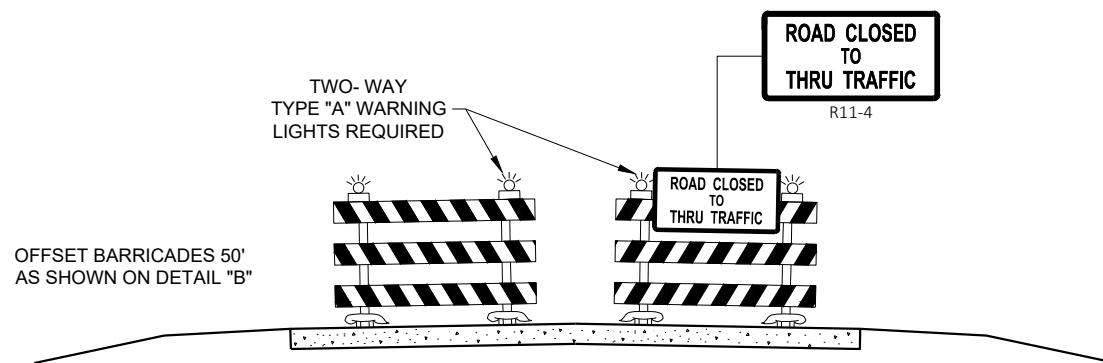
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020 /S/ Andrew Heidtke
DATE DATE WORK ZONE ENGINEER

FHWA



**DETAIL D
ROAD CLOSURE BARRICADE DETAIL
APPROACH VIEW**



**DETAIL E
LANE CLOSURE BARRICADE DETAIL
APPROACH VIEW**

SEE SDD 15C2 - SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW - INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11 - 2, R11 - 3, M4 - 9, R11 - 4, AND R10 - 61 SIGNS PLACED ON THE BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE RAIL OR BOTTOM RAILS.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

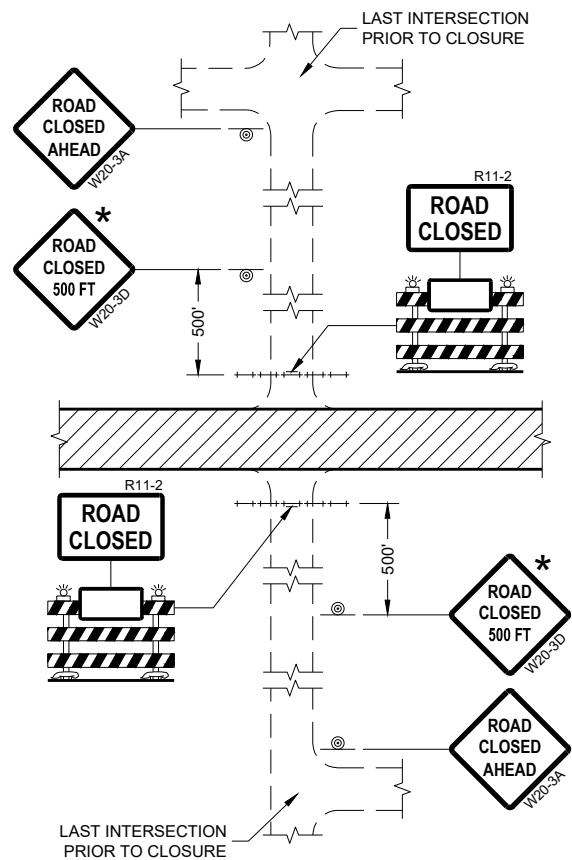
- R11 - 2 SHALL BE 48" X 30"
- R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60" X 30"
- M4 - 9 SHALL BE 30" X 24"
- M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1 - 1 SHALL BE 36" X 36"

- ① TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT SPACING).
- ② THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.
- ③ FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".
- ④ FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".
- ⑤ FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.
- ⑥ INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.
- ⑦ "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

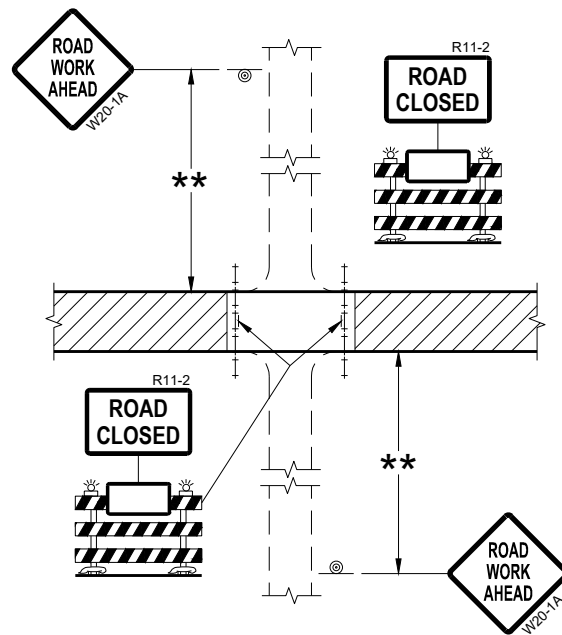
**BARRICADES AND SIGNS
FOR
VARIOUS CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

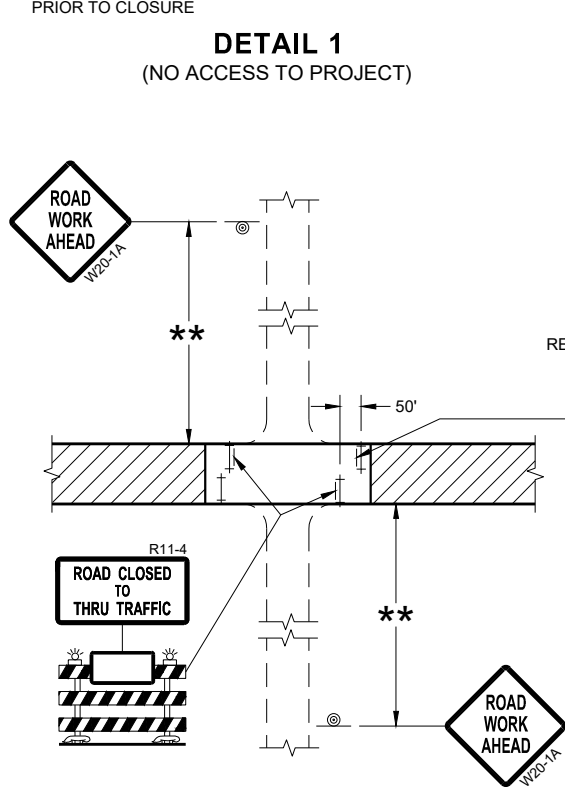
APPROVED
February 2020 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER



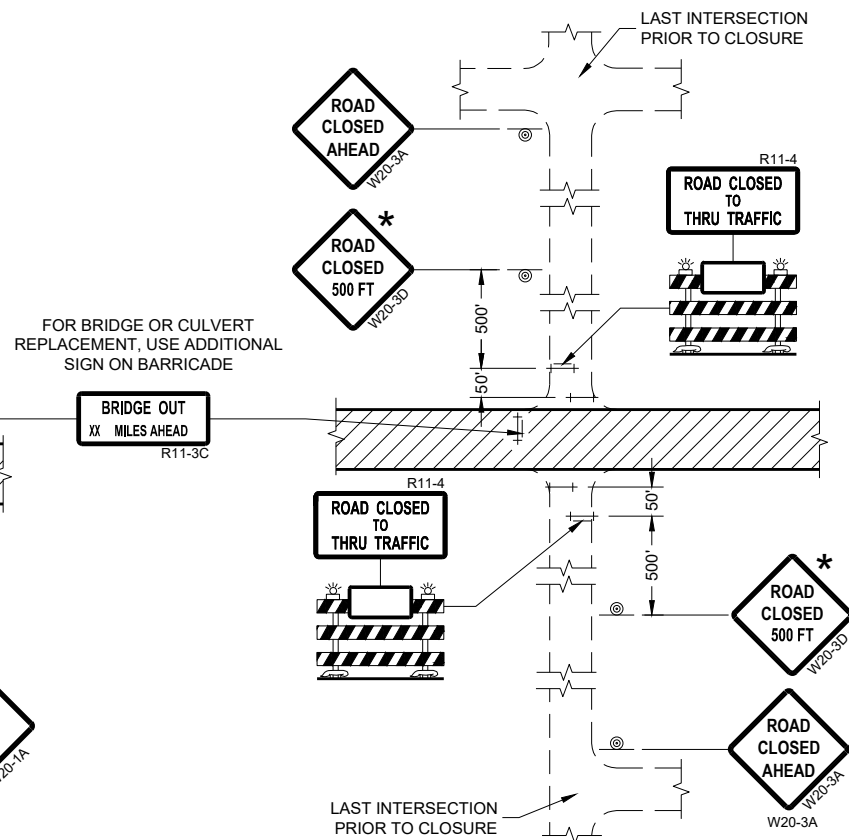
DETAIL 1
(NO ACCESS TO PROJECT)



DETAIL 2
(PUBLIC CROSS-TRAFFIC MAINTAINED.
NO ACCESS TO PROJECT)



DETAIL 3
(PUBLIC CROSS-TRAFFIC MAINTAINED.
CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)



DETAIL 4
(CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE (500 FEET DESIRABLE) TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3, AND R11-4 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
R11-2 SHALL BE 48" X 30".
R11-4 AND R11-3 SHALL BE 60" X 30".

- * OMIT THE "ROAD CLOSED 500 FT." SIGN IF THE LAST INTERSECTION IS 500 FEET OR LESS FROM THE WORK ZONE.
- ** 500' MAX. OR AT LAST INTERSECTION, WHICHEVER IS CLOSEST.

LEGEND

- ⊙ SIGN ON PERMANENT SUPPORT
- ⊥ TYPE III BARRICADE
- ⊥ TYPE III BARRICADE WITH ATTACHED SIGN
- ⚡ TYPE "A" WARNING LIGHT (FLASHING)
- ▨ WORK AREA

**BARRICADES AND SIGNS
FOR
SIDEROAD CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
July 2018 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.




ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

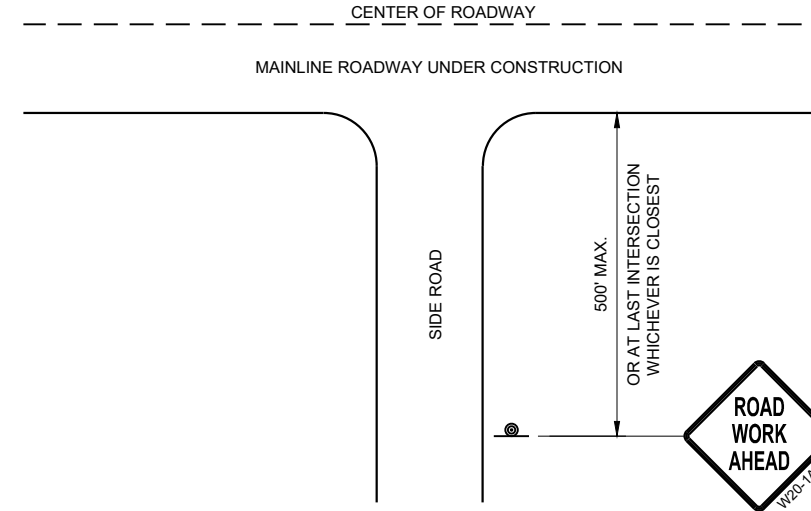
SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

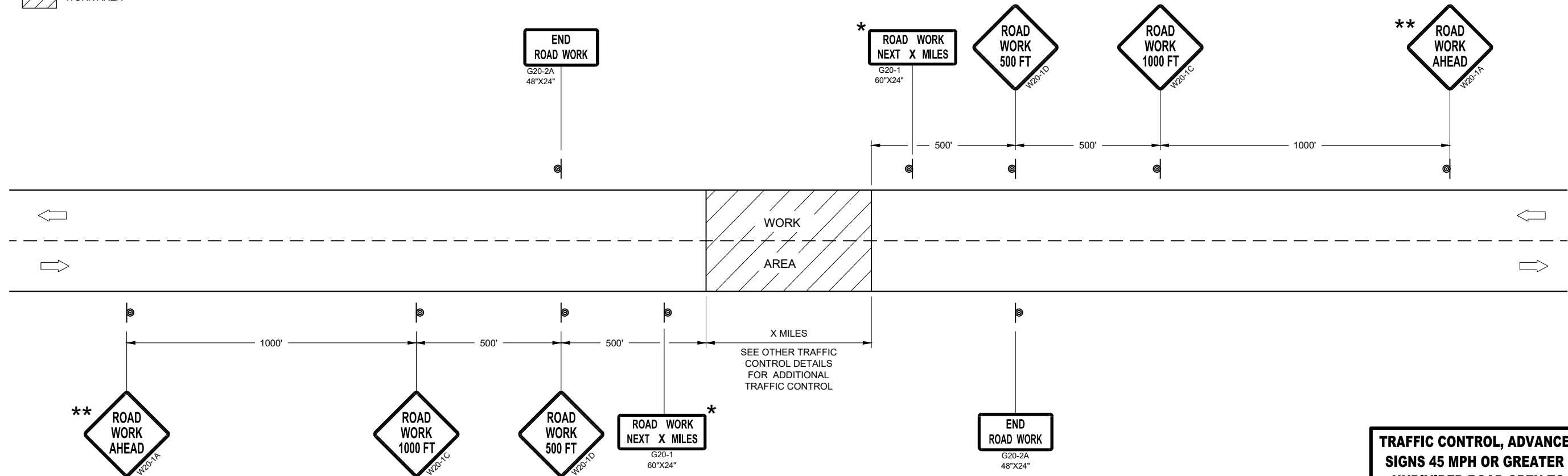
- * OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS
- ** PLACE AN ADDITIONAL W20-1A "ROAD WORK AHEAD" SIGN IF WORK AREA WITHIN THE PROJECT IS SEPARATED BY MORE THAN 2 MILES FROM PREVIOUS WORK AREA.

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC
-  WORK AREA



TYPICAL SIDE ROAD APPROACH WARNING SIGN DETAIL



TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45MPH OR GREATER

TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 MPH OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE July 2018 /S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.




ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS, 36"X36" SIGNS MAY BE USED INSTEAD OF 48" X 48" SIGNS.

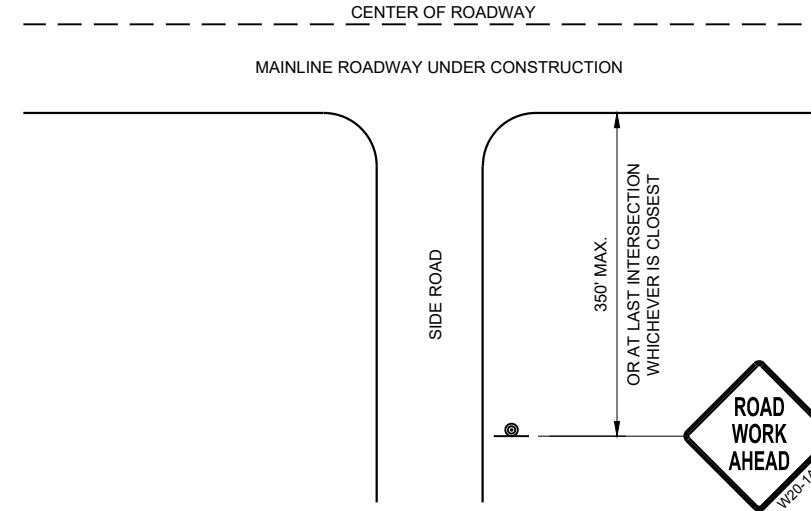
SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

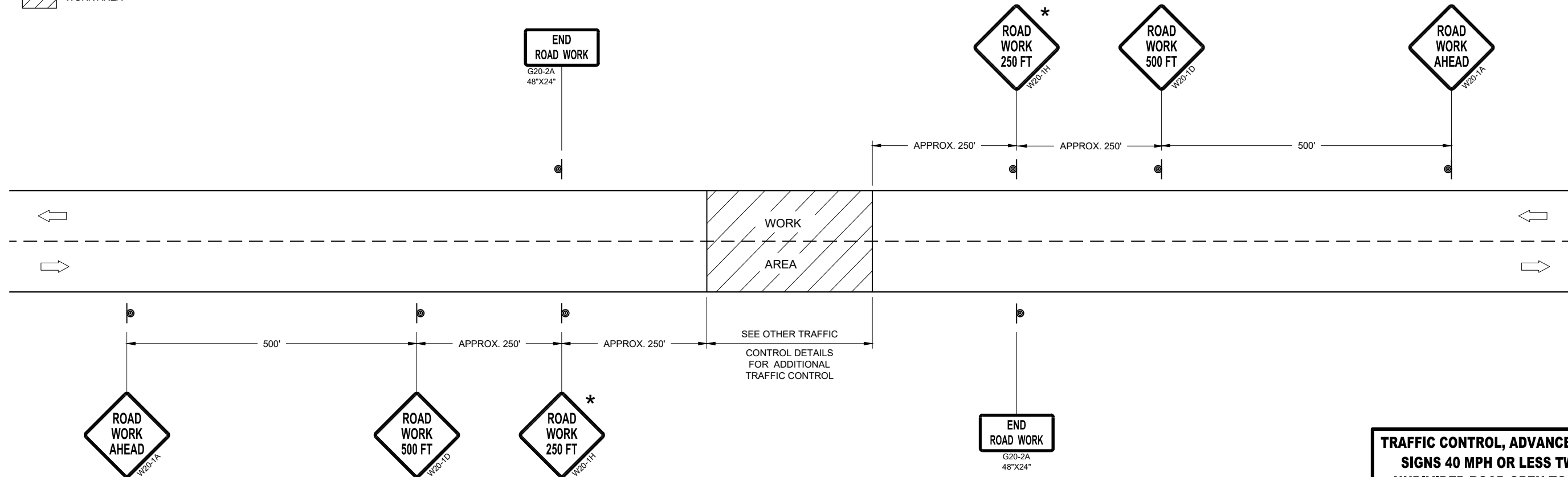
* THE THIRD W20-1 SIGN IS REQUIRED ONLY IF THERE IS AN INTERSECTION BETWEEN THE "ROAD WORK 500 FEET" SIGN AND THE WORK ZONE. ADJUST THE PLACEMENT OF THIS SIGN BASED ON INTERSECTION LOCATION AND OTHER FIELD CONDITIONS.

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC
-  WORK AREA



**TYPICAL SIDE ROAD APPROACH
WARNING SIGN DETAIL**



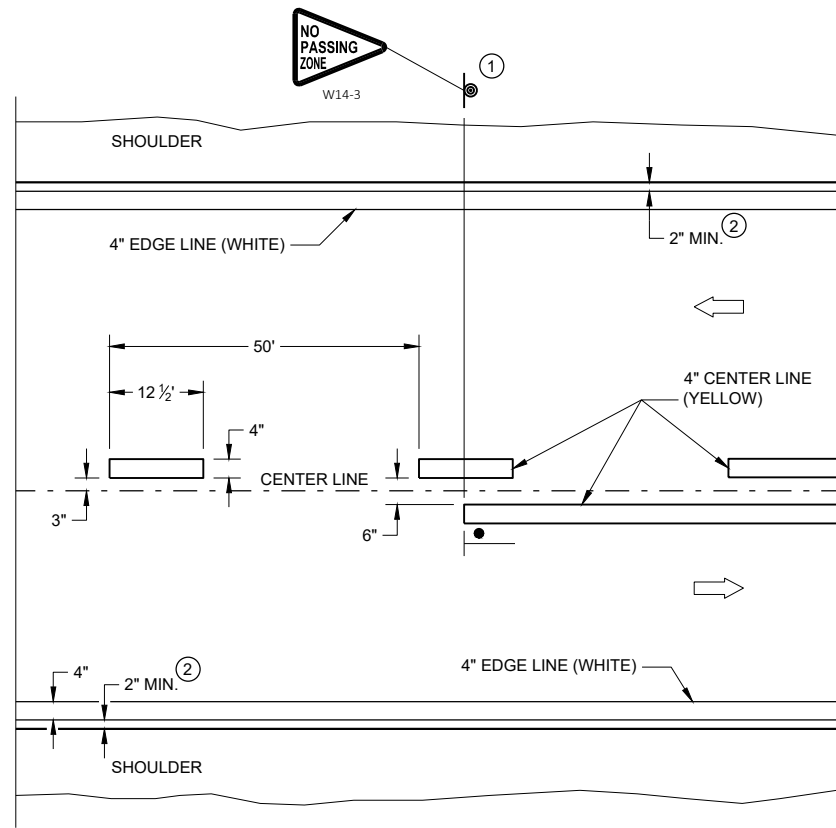
TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40MPH OR LESS

**TRAFFIC CONTROL, ADVANCE WARNING
SIGNS 40 MPH OR LESS TWO-WAY
UNDIVIDED ROAD OPEN TO TRAFFIC**

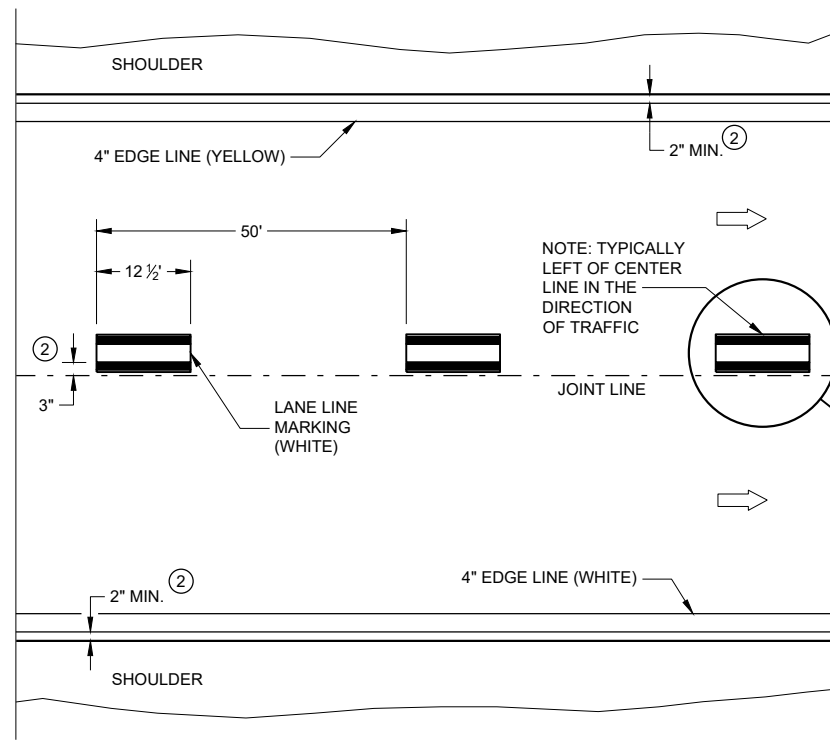
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE July 2018 /S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

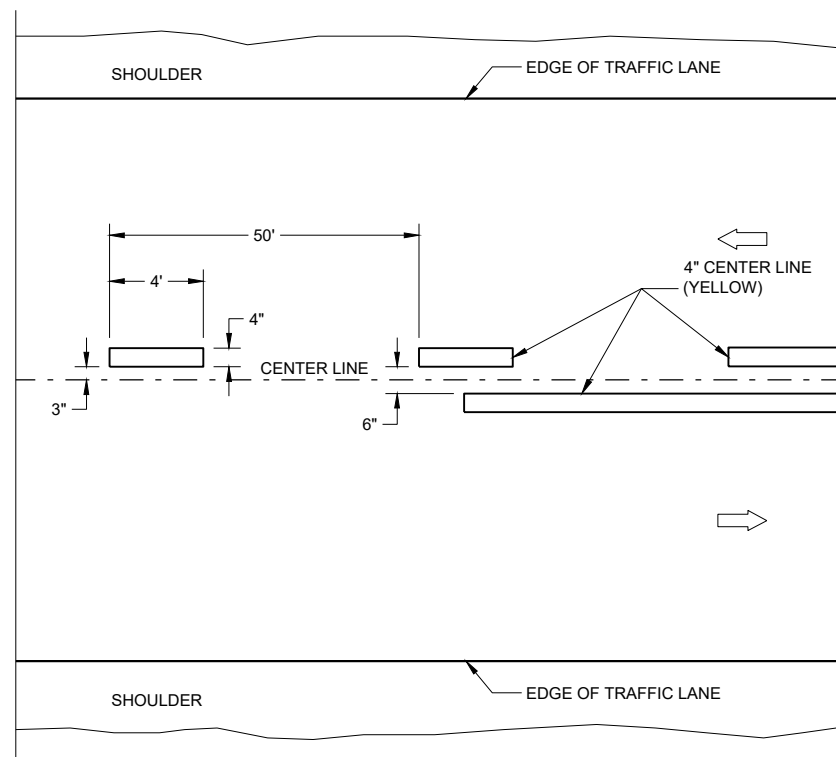


TWO WAY TRAFFIC

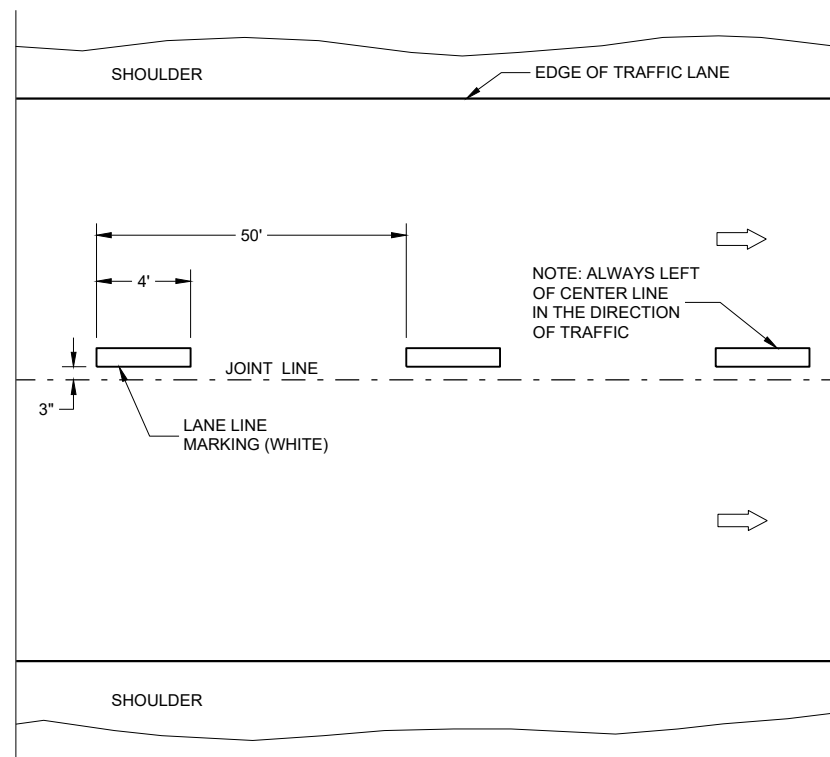


ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING



TWO WAY TRAFFIC



ONE WAY TRAFFIC

TEMPORARY PAVEMENT MARKING

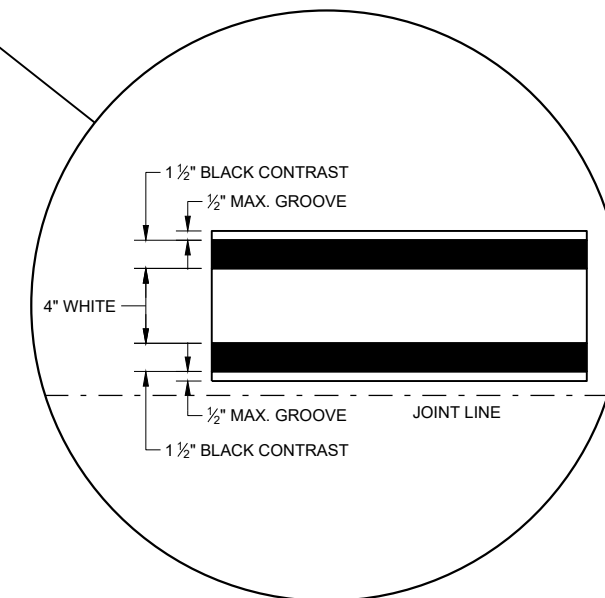
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

- ① LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING
- ② MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

LEGEND

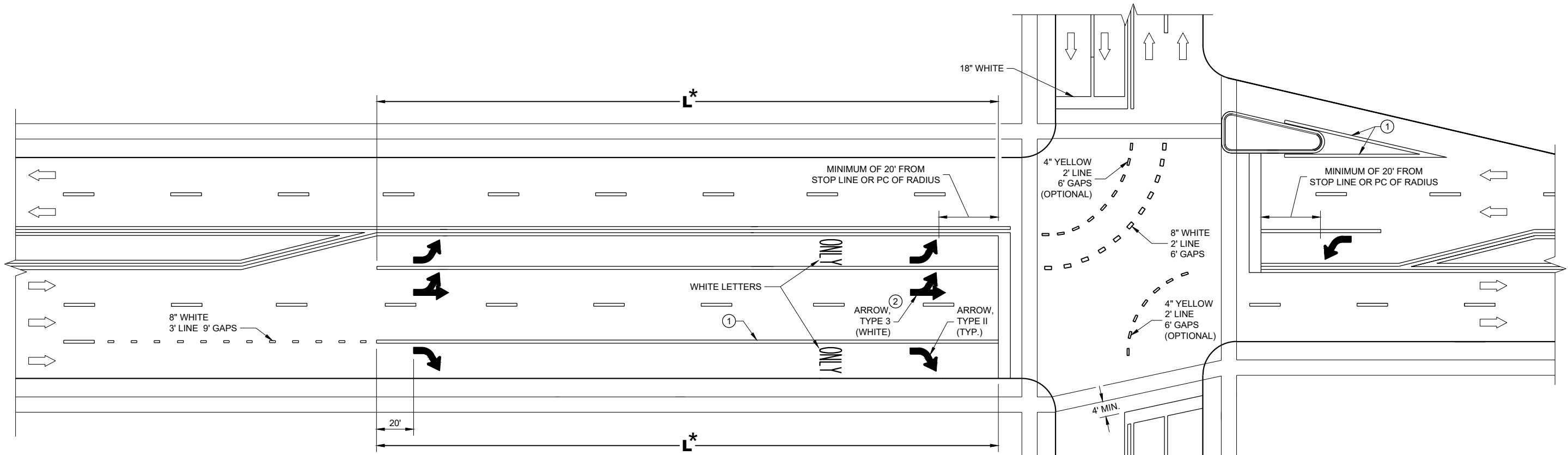
- |• "T" MARKING
- ⊙ SIGN ON PERMANENT SUPPORT
- DIRECTION OF TRAFFIC



**LONGITUDINAL MARKING
(MAINLINE)**

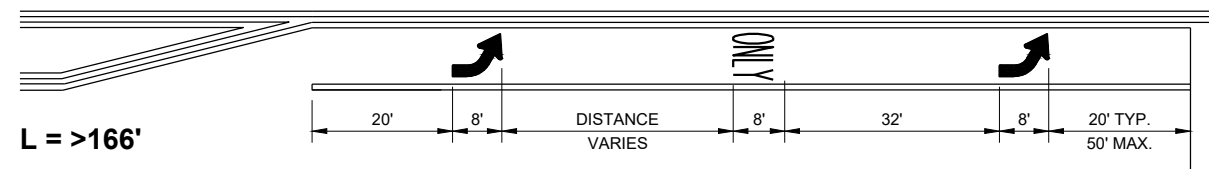
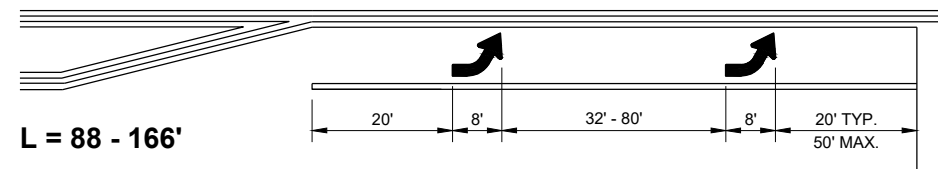
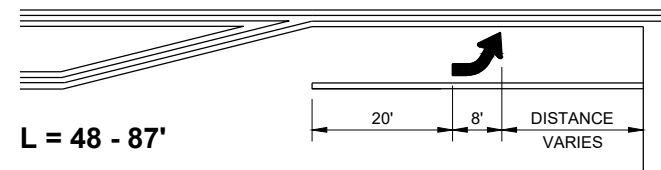
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020 /S/ Matthew Rauch
DATE STATEWIDE SIGNING AND MARKING
ENGINEER



TURN LANE OPTIONS

LENGTH OF TURN BAY (**L**) OF 0 - 47' DOES NOT REQUIRE PAVEMENT MARKING ARROWS OR WORDS



*(SEE TURN LANE OPTIONS FOR PLACEMENT OF PAVEMENT MARKING ARROWS AND WORDS)

GENERAL NOTES

- ① 8" WHITE
- ② QUANTITY AND LOCATION OF TYPE 3 ARROWS ARE THE SAME AS THE TYPE II ARROWS IN THE ADJACENT TURN LANE. FOR TURN LANES WITH A PHYSICAL SEPARATION IN THE SAME DIRECTION OF TRAVEL, THE ARROWS AND "ONLY" MARKING MAY BE ELIMINATED.






➡ DIRECTION OF TRAFFIC

L = LENGTH OF TURN BAY

PAVEMENT MARKING (TURN LANES)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

LEGEND

-  SIGN ON PORTABLE OR PERMANENT SUPPORT
-  TEMPORARY PORTABLE RUMBLE STRIP ARRAY
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS, DEVICES, AND LOCATION OF ALL FLAGGERS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

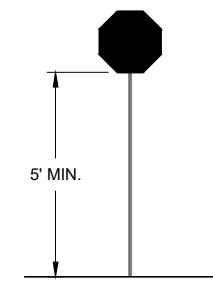
WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGING

- FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT REMOVE TEMPORARY PORTABLE RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.
- ① FOR MOVING WORK OPERATIONS, POST ADDITIONAL W20-7A FLAGGER SIGNS AT APPROXIMATELY 3,500' INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.
 - ② SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.
- WHEN THE DISTANCE BETWEEN FLAGGERS EXCEEDS 2 MILES, A PILOT CAR IS REQUIRED. WHEN CURVES REDUCE SIGHT DISTANCE BELOW 400', A PILOT CAR IS REQUIRED.

TEMPORARY PORTABLE RUMBLE STRIPS

- UTILIZE TEMPORARY PORTABLE RUMBLE STRIPS ON ALL FLAGGING OPERATIONS.
- ③ EACH TEMPORARY PORTABLE RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS SPACED ACCORDING TO MANUFACTURER'S RECOMMENDATION, PLACED TRANSVERSE ACROSS THE LANE AT LOCATIONS SHOWN.
- ONLY USE TEMPORARY PORTABLE RUMBLE STRIPS FOR THE APPROVED PRODUCTS LIST.
- INSTALL TEMPORARY RUMBLE STRIPS PER MANUFACTURER'S RECOMMENDATIONS.
- PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.
- DO NOT INSTALL TEMPORARY PORTABLE RUMBLE STRIPS ON GRAVEL, MILLED SURFACES, OR ASPHALT THAT HAS BEEN PAVED LESS THAN 12 HOURS.



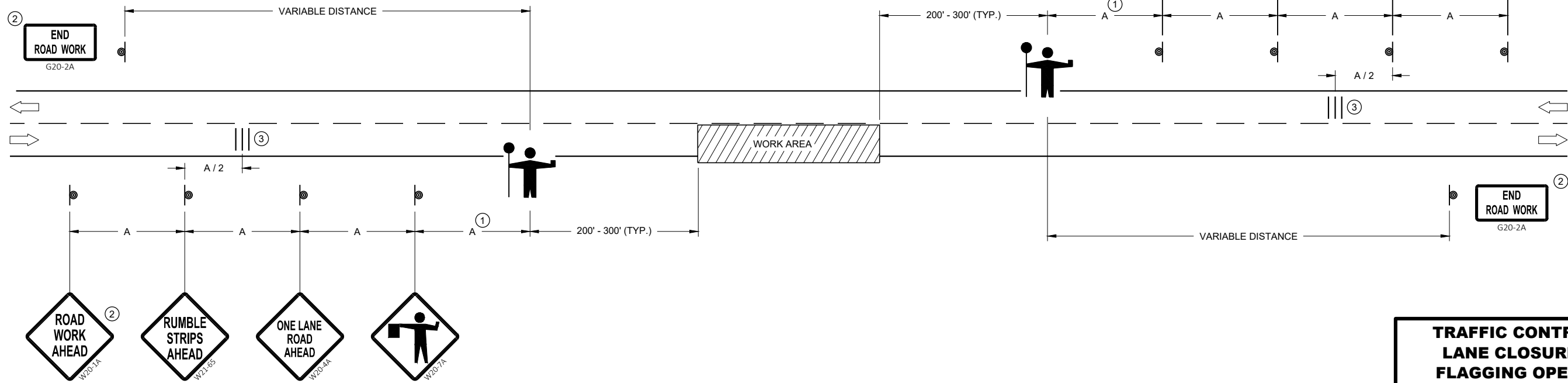
STOP/SLOW PADDLE ON SUPPORT STAFF

SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE

SPEED LIMIT	SPACING "A"
25-30 MPH	200'
35-40 MPH	350'
45-55 MPH	500'



USE OF W03-4 SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7A AND W20-4A SIGNS, USING SPACING "A".



TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION





TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE May 2019 /S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

LEGEND

- V1 LEAD VEHICLE
- V2 MARKING VEHICLE
- V3 SHADOW VEHICLE
-  TRUCK MOUNTED ATTENUATOR (TMA)
-  SIGN ON TEMPORARY SUPPORT
-  DIRECTION OF TRAFFIC
-  FLASHING ARROW PANEL (CAUTION)

GENERAL NOTES

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

ALL VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL OPERATING IN CAUTION MODE. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.

DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL AND HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.

THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS.

WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR LAY STATIONARY SIGNS AND SUPPORTS FLAT ON THE GRADE WITH UPRIGHTS ORIENTED PARALLEL TO AND DOWNSTREAM FROM TRAFFIC.

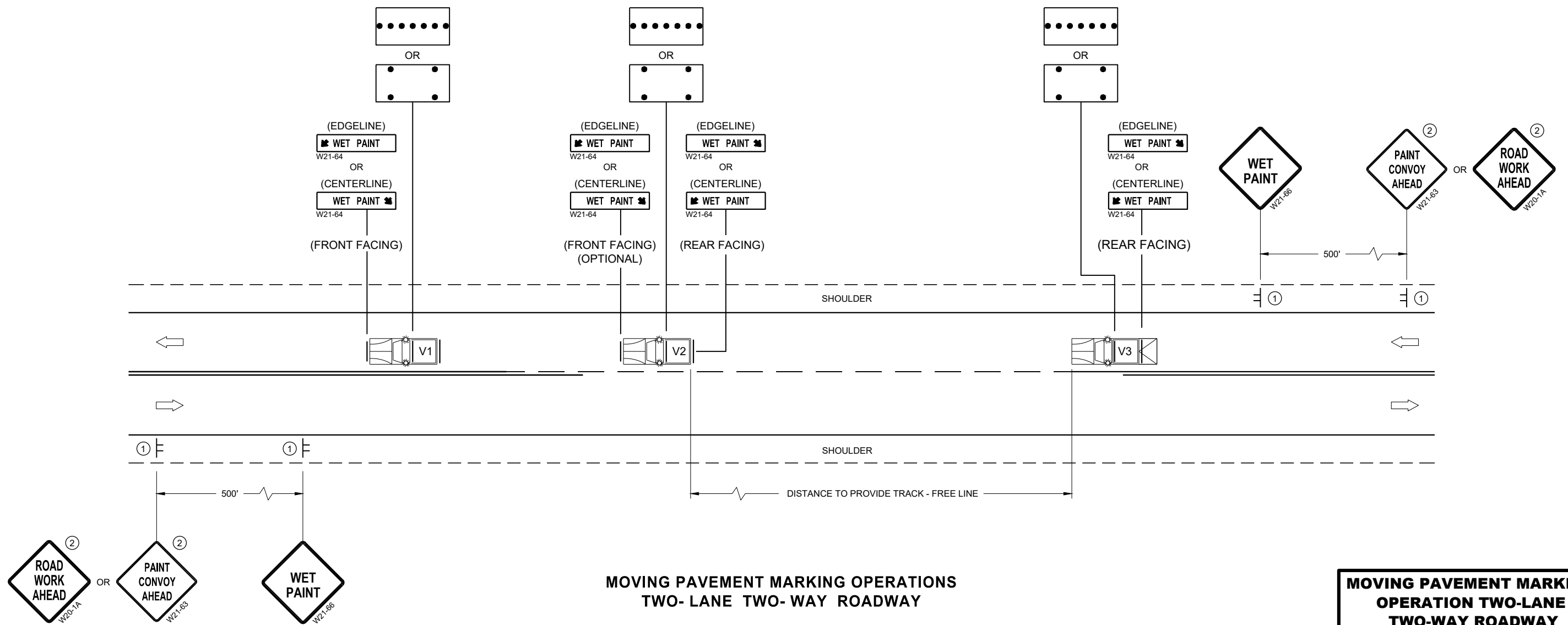
CONES SHOULD BE USED BETWEEN THE MARKING AND SHADOW VEHICLE AT 100 FOOT SPACING. CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.

CONES SHALL BE A MINIMUM OF 28" FOR WET PAVEMENT MARKING.

- ① SIGNS SHALL BE REPEATED APPROXIMATELY EVERY THREE MILES.
- ② IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1A OR W21-63 ARE NOT REQUIRED.

6

6



**MOVING PAVEMENT MARKING OPERATIONS
TWO-LANE TWO-WAY ROADWAY**

SDD 15C19 - 06a

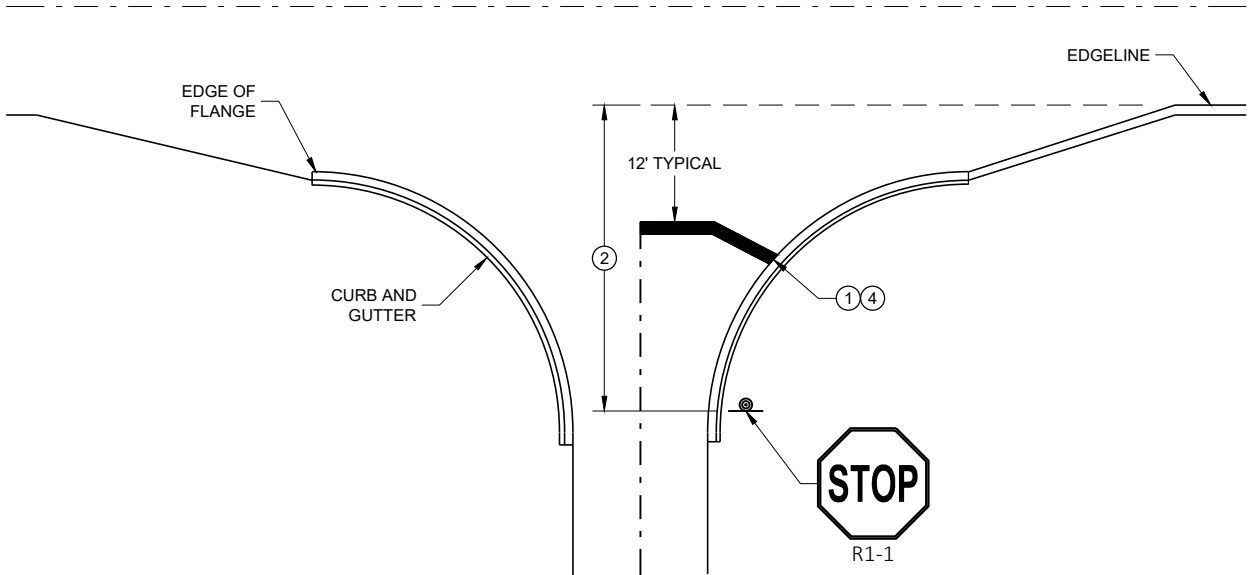
SDD 15C19 - 06a

MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED November 2019 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

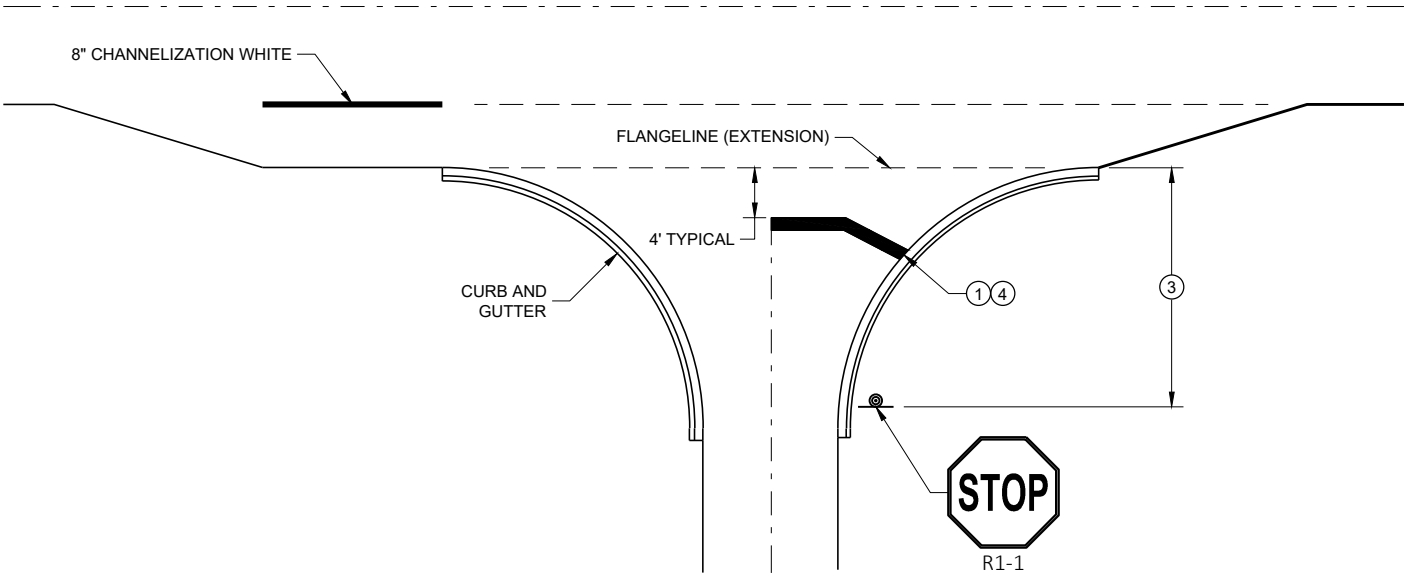
GENERAL NOTES

STOP SIGN SHALL BE PLACED A MINIMUM OF 6 FEET TO A MAXIMUM OF 50 FEET FROM THE EDGELINE LOCATION.

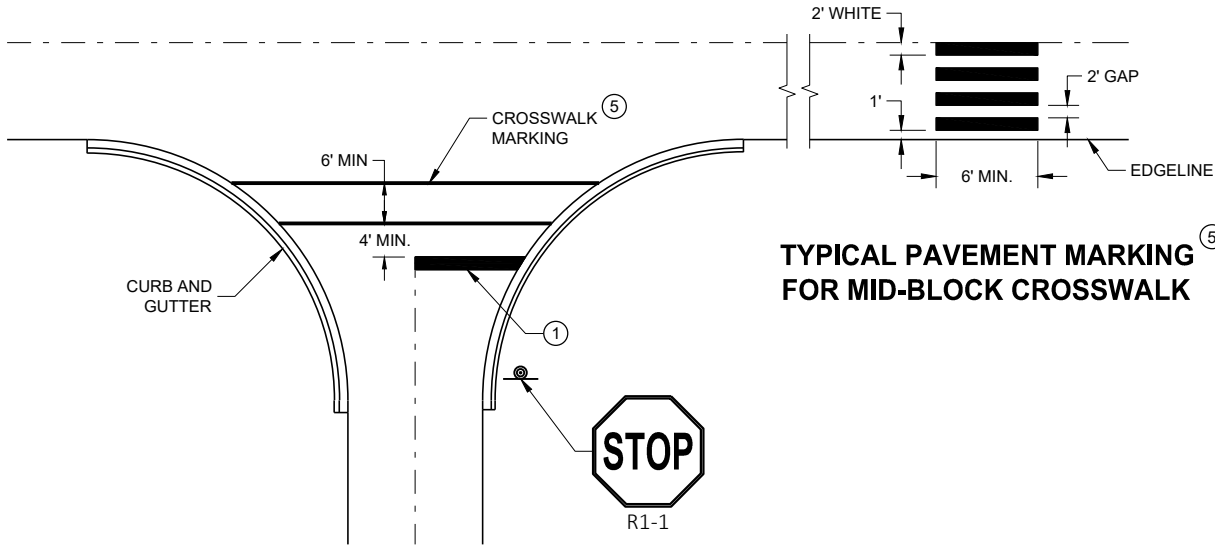
- ① 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE REGION MARKING ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
- ② NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGELINE.
- ③ NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGELINE EXTENSION.
- ④ MOVE CLOSER TO THE EDGE OF TRAVEL LINE AS NEEDED FOR VISIBILITY AND SIGHT LINES (NO CLOSER THAN 4 FEET).
- ⑤ LADDER BAR CROSSWALKS SHOULD ONLY BE USED FOR MID BLOCK CROSSINGS. USE 2 - 6" TRANSVERSE LINES INSTEAD.



TYPICAL STOP LINE PAVEMENT MARKING WITH CURB AND GUTTER

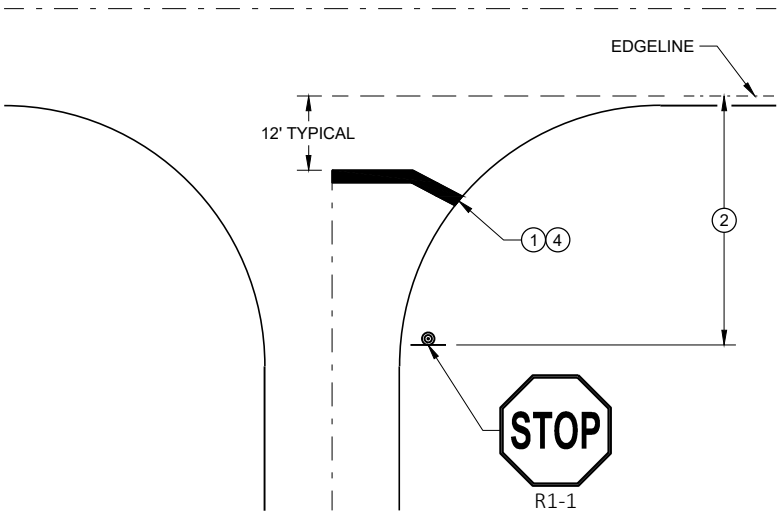


TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH RIGHT TURN LANE



TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH CROSSWALK MARKING

TYPICAL PAVEMENT MARKING FOR MID-BLOCK CROSSWALK



TYPICAL STOP LINE PAVEMENT MARKING WITHOUT CURB AND GUTTER

STOP LINE AND CROSSWALK PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2019 /S/ Matthew Rauch
DATE STATE SIGNING AND MARKING ENGINEER

FHWA

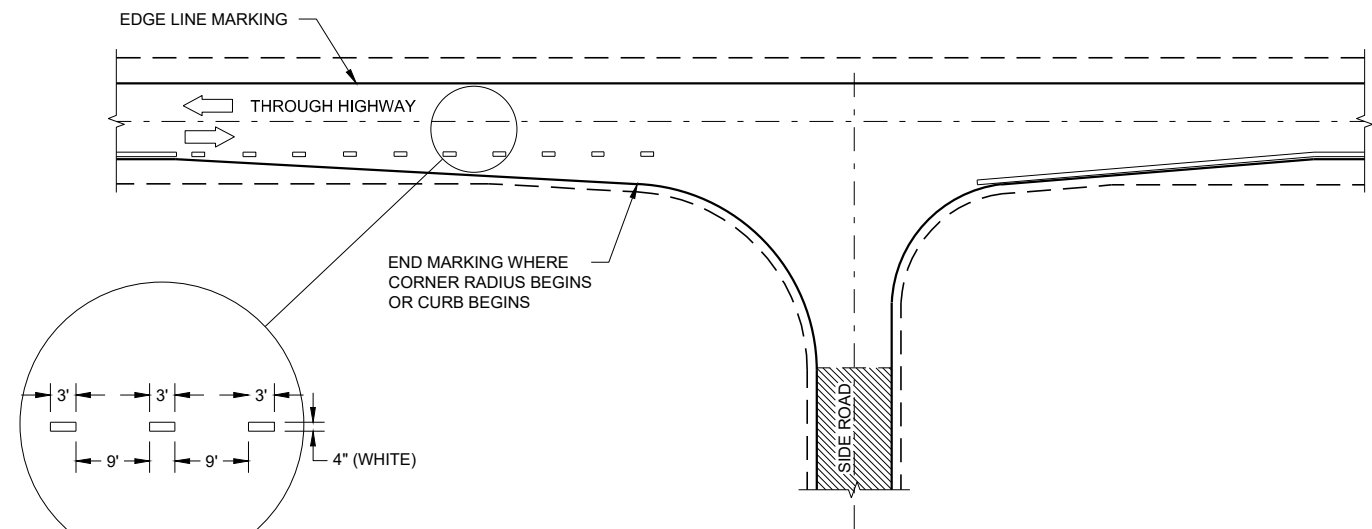
GENERAL NOTES

OMIT EDGE LINES THROUGH INTERSECTIONS. CONTINUE EDGE LINES THROUGH DRIVEWAYS.

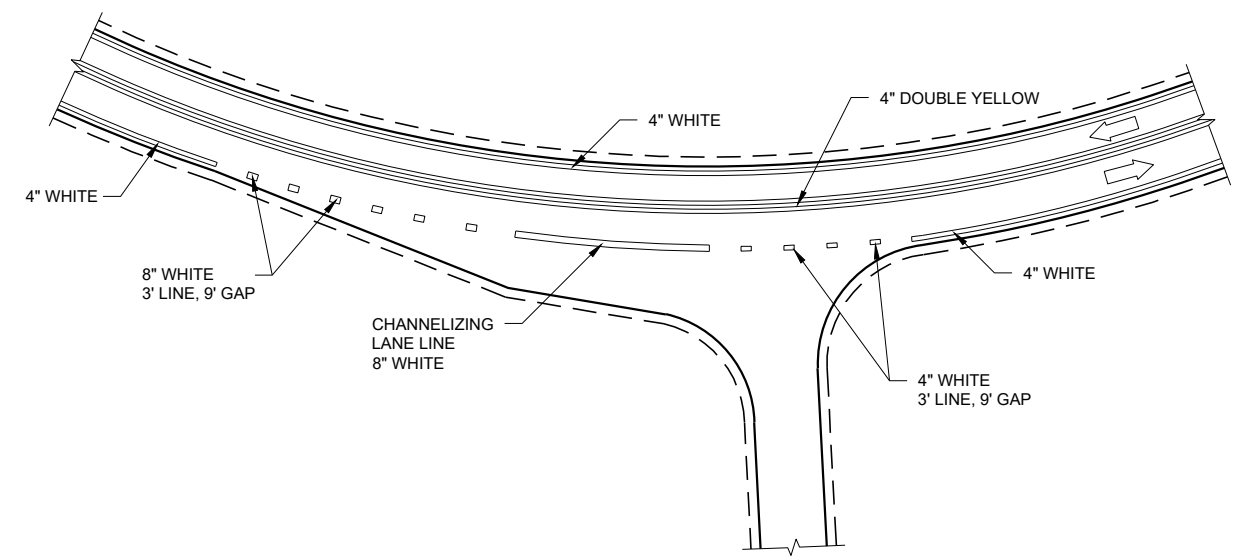
- ① WHEN DISTANCE "A" IS LESS THAN 250 FEET, OMIT LANE LINE.
- ② WHEN DISTANCE "B" IS LESS THAN 100 FEET, OMIT CHANNELIZING LANE LINE.
- ③ BARRIER LINE ENDS AT SIDE ROAD PAVEMENT / SURFACE EDGE EXTENSION.
- ④ BARRIER LINE STARTS 500 FEET PRIOR TO THE BYPASS TAPER.

LEGEND

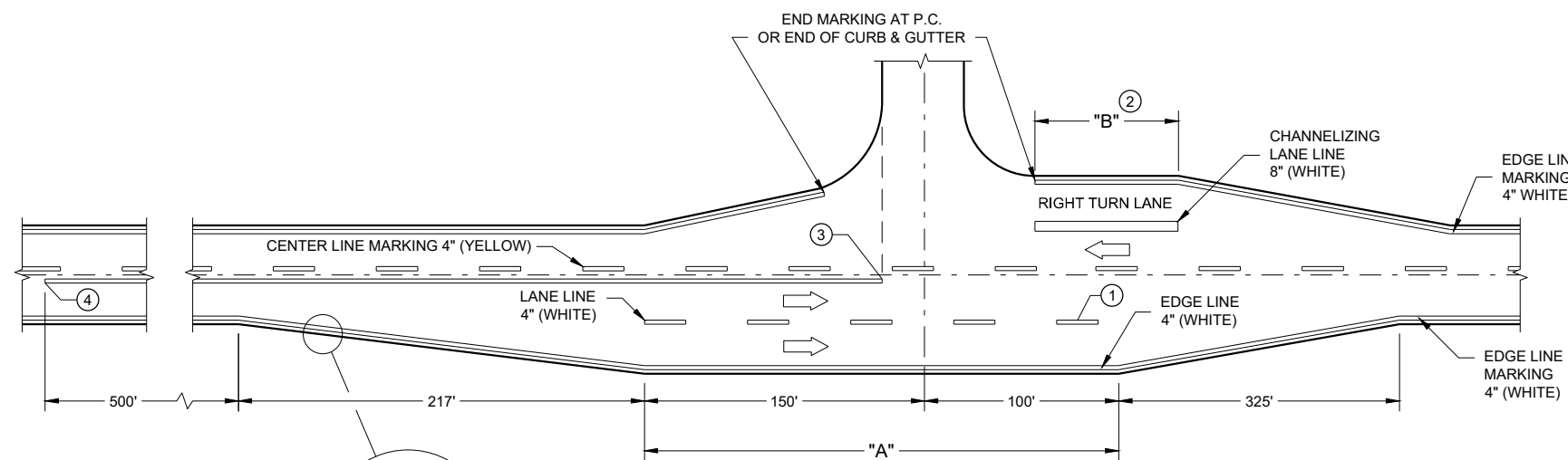
➡ DIRECTION OF TRAVEL



MINOR INTERSECTION



INTERSECTION ON OUTSIDE OF CURVE







**MAJOR INTERSECTIONS
(INTERSECTION WITH FULL RIGHT TURN LANE OR BYPASS LANE)**

**PAVEMENT MARKING
(INTERSECTIONS)**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  DIRECTION OF TRAFFIC
-  WORK ZONE

GENERAL NOTES

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" SIGNS MAY BE USED IF APPROVED BY THE REGIONAL TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

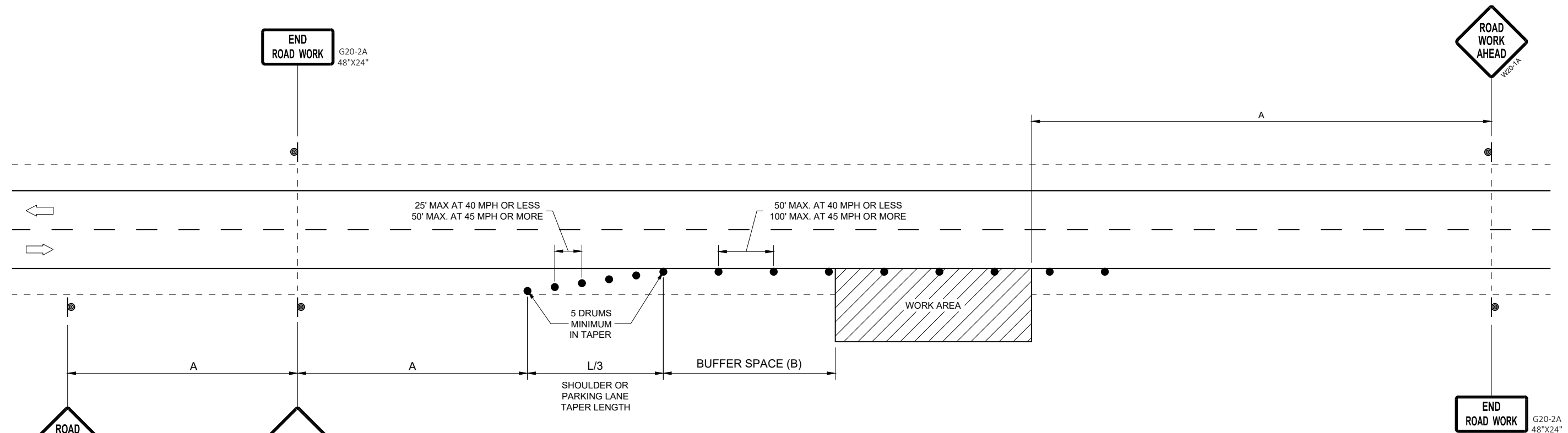
CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

W20-1A AND G20-2A SIGNS ARE NOT REQUIRED IF THE WORK AREA IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT. G20-2A SIGNS MAY ALSO BE OMITTED IF DURATION OF WORK IS LESS THAN 7 CONTINUOUS DAYS AND NIGHTS.

6

6



POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET	SHOULDER TAPER L / 3 W, LATERAL OFFSET (FT)						BUFFER SPACE (B) FEET
		3	4	5	6	7	8	
25	200'	10	14	17	21	24	28	55
30	200'	15	20	25	30	35	40	85
35	350'	20	27	34	40	47	54	120
40	350'	26	35	44	53	62	70	170
45	500'	45	59	74	89	104	119	220
50	500'	50	66	83	99	116	132	280
55	500'	54	73	91	109	127	145	335'

OR
IF TRAFFIC CONTROL DEVICES
ENCROACH ONTO TRAVELED WAY, USE



TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2020 /S/ Andrew Heidtke
DATE STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER

FHWA

SDD 15D28 - 04

SDD 15D28 - 04

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

DETAILED DRAWINGS OF PROPOSED ALTERNATE DESIGNS FOR METAL MONUMENTS OR MONUMENT COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.

PERMANENT MAGNETS SHALL BE INSERTED NEAR THE TOP AND BOTTOM OF ALL ALUMINUM MONUMENTS SO THE MONUMENT CAN EASILY BE DETECTED BY A METAL DETECTOR.

THE CAST IRON MONUMENT COVER SHALL BE A "NON-ROCKING" TYPE. ADJUSTMENT OF THE COVER TO GRADE MAY BE ACCOMPLISHED BY THE USE OF MORTAR AND BRICK, OR BY EITHER PRECAST OR CAST-IN-PLACE REINFORCED CONCRETE GRADE RINGS.

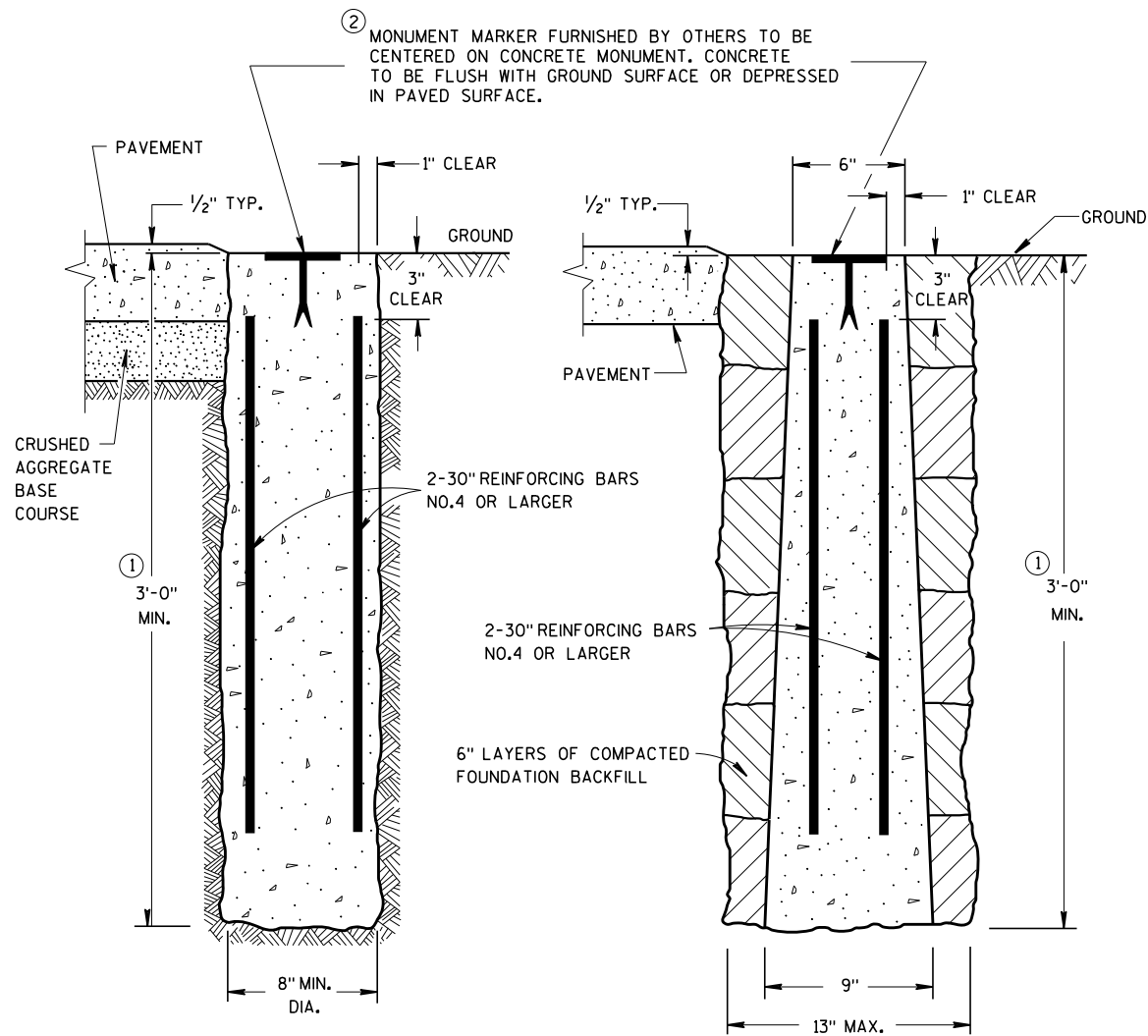
MONUMENTS SHALL BE LOCATED AND PLACED AT THE DIRECTION OF THE ENGINEER.

ALUMINUM MONUMENTS AND MONUMENT COVERS SHALL BE MADE FROM AN ALUMINUM AND MAGNESIUM ALLOY AS DETERMINED BY THE MANUFACTURER.

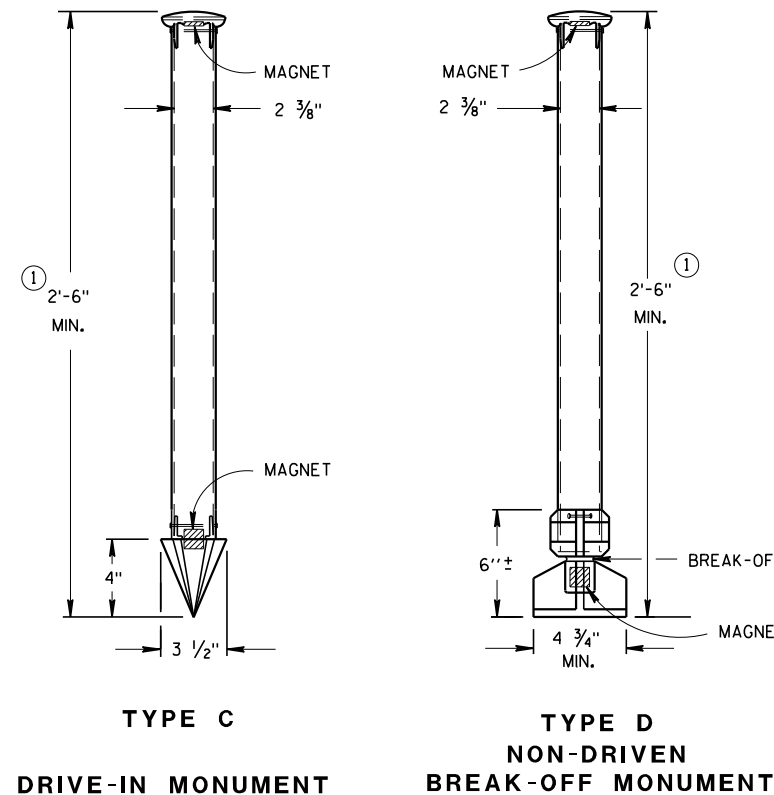
THE MONUMENT COVERS DETAILED ON THIS DRAWING ARE NOT EQUAL ALTERNATES. MONUMENT COVERS SHALL BE CAST IRON UNLESS ALUMINUM IS SPECIFIED ELSEWHERE IN THE CONTRACT.

MONUMENT SHALL BE CAST-IN-PLACE CONCRETE UNLESS PRECAST CONCRETE OR ALUMINUM MONUMENTS ARE SPECIFIED IN THE CONTRACT OR PERMITTED BY THE ENGINEER

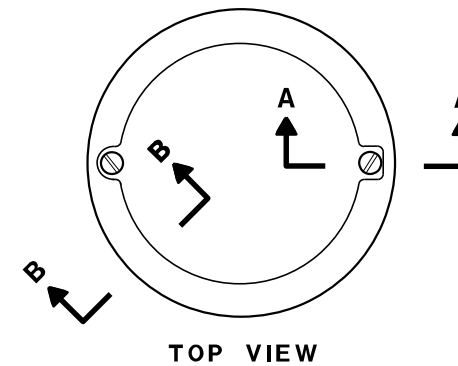
- ① MINIMUM LENGTH SHALL BE 4'-0" FOR MONUMENTS INSTALLED IN PAVED AREAS.
- ② AN OFFICIAL COUNTY MONUMENT MARKER SUPPLIED BY A COUNTY MAY BE REQUIRED FOR SOME SECTION CORNERS AND WITNESS MONUMENTS INSTEAD OF THIS WIS DOT MARKER.



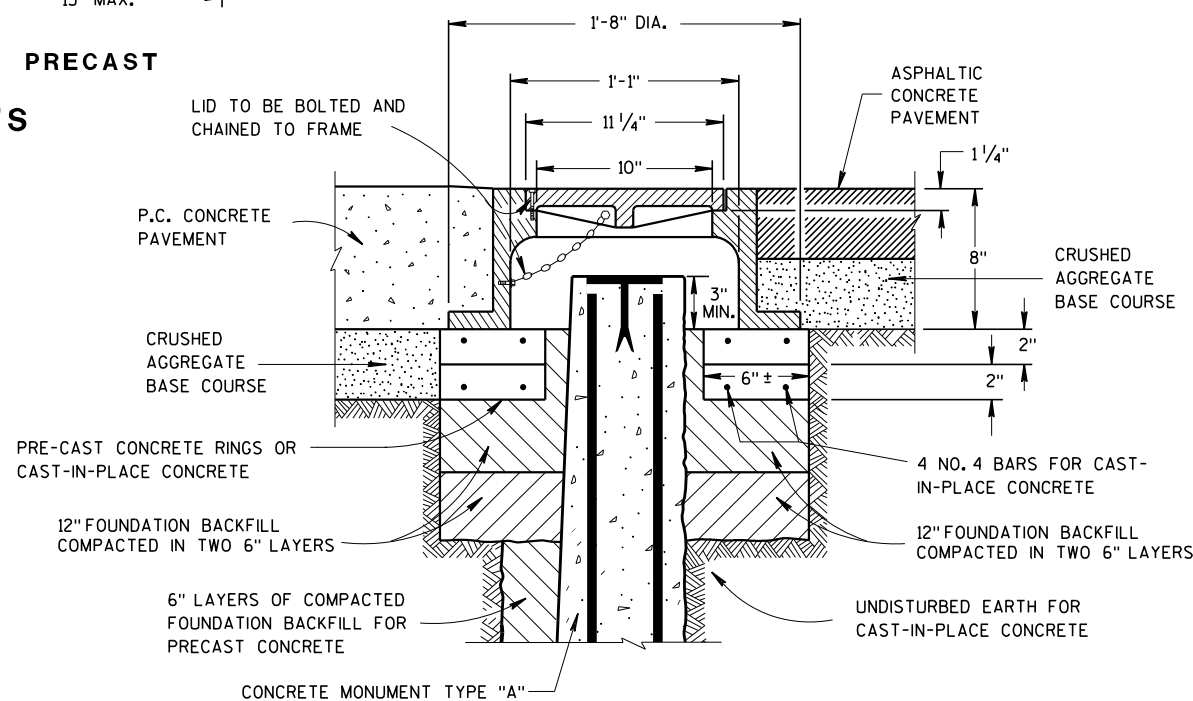
**CAST-IN-PLACE
CONCRETE MONUMENTS
TYPE A**



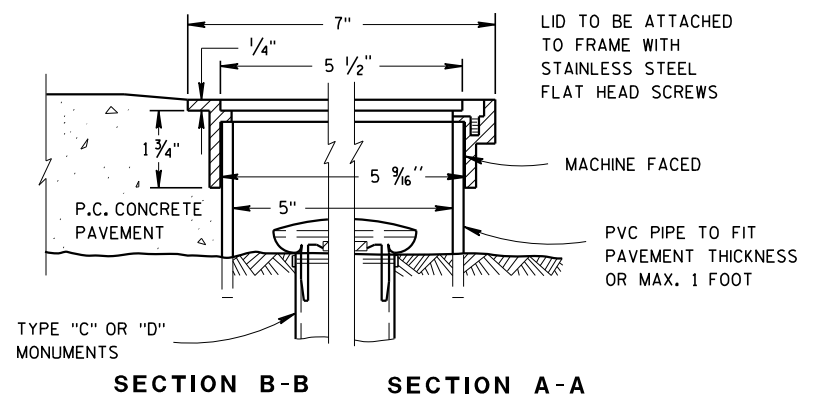
**ALUMINUM MONUMENTS
(INCLUDES MARKER)**



TOP VIEW

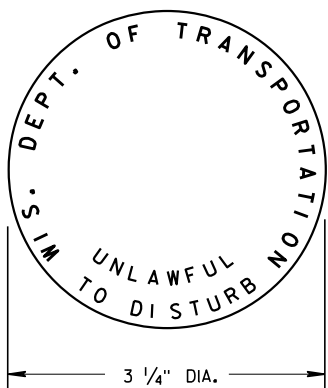


**CAST IRON MONUMENT COVER
(APPROXIMATE WEIGHT 95 LBS)**



ALUMINUM MONUMENT COVER

(APPROXIMATE WEIGHT 2 LBS)
(FOR CONCRETE PAVEMENT ONLY)



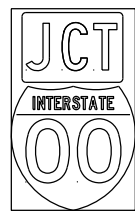
**WIS DOT MONUMENT
MARKER LOGO
FOR TYPES "A", "C", & "D"**

**LANDMARK REFERENCE
MONUMENTS AND COVERS**

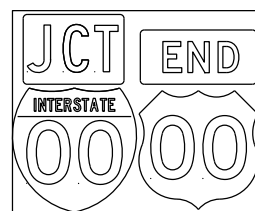
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
March 2018 /S/ Raymond A. Kumapayi
DATE CHIEF SURVEYING AND MAPPING ENGINEER
FHWA

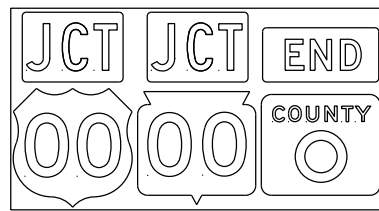
TYPICAL ASSEMBLIES



J1-1



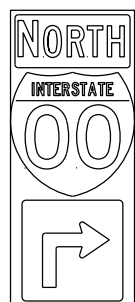
J1-2



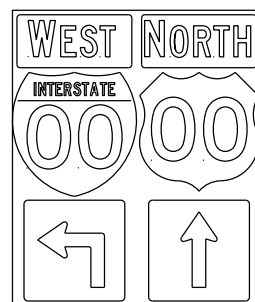
J1-3



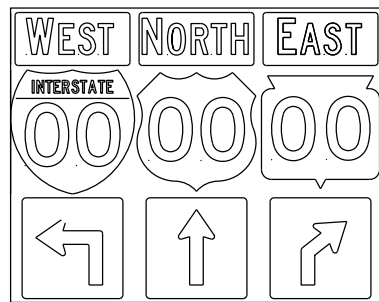
JR1-1



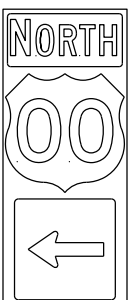
J2-1



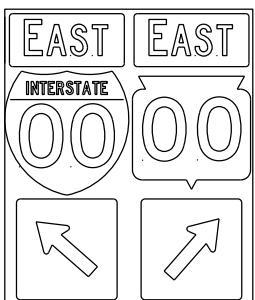
J2-2



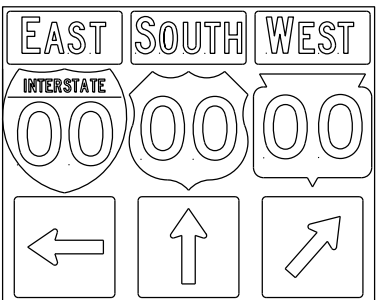
J2-3



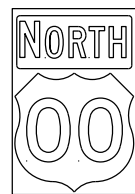
J3-1



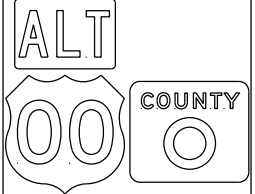
J3-2



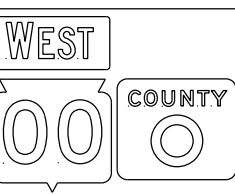
J3-3



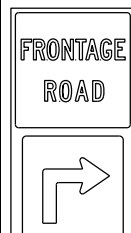
J4-1



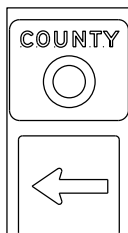
J4-2



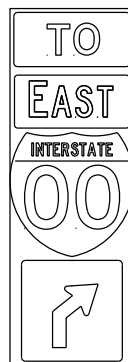
J4-2



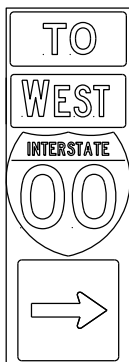
J12-1



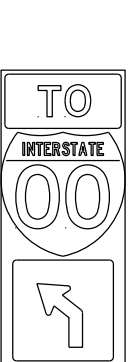
J13-1



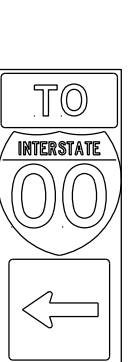
J32-1



J33-1



J22-1



J23-1



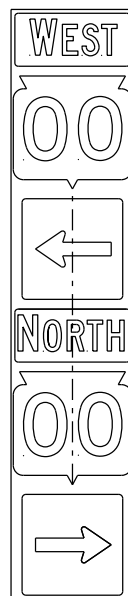
JR13-1



JR23-1

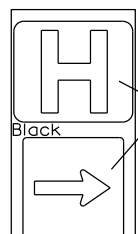


JR99-1



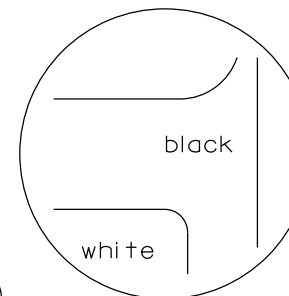
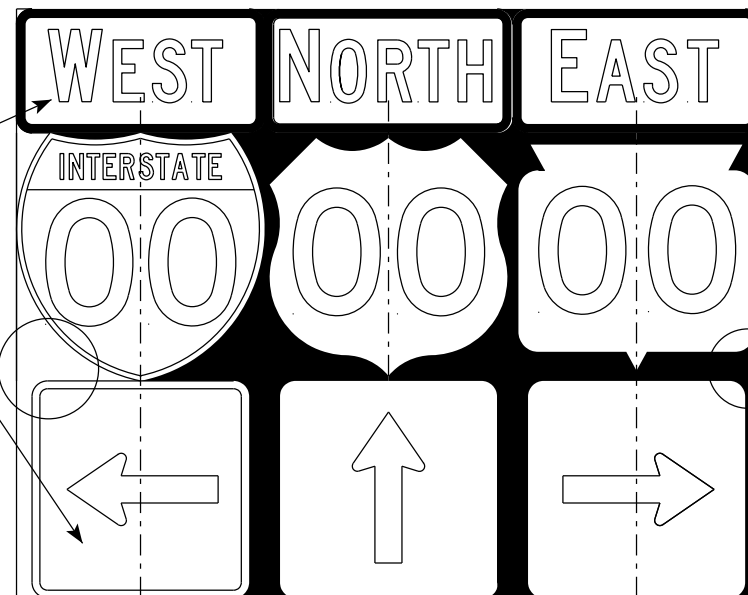
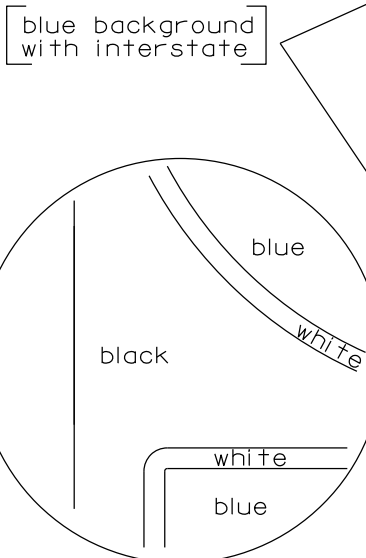
JV

(Typical Vertical J-Assembly See Note 10 and 11)



JH-1

Blue Background



black background

NOTES

- Signs are Type II - Type H Reflective
- Color:
 - Background - Black Non-reflective
 - Message - see Note 5
- Message Series - See Note 5
- Corners shall be square or rounded if base material is plywood. If base material is metal the corners shall be rounded.
- The colors and message spacing on each marker shall be according to the applicable route marker panel specifications.
- Certain marker heads require the component pieces to be the same color. As an example, all the components used with an MI-1 Interstate marker shall be blue.
- Single panel j-assemblies shall only be used with route marker shields that are same size. If the route marker shields are different size use multiple piece component.
- Route assemblies that have 24 inch route shields and have dimensions greater than 48 inches (both vertical and horizontal) shall have one horizontal splice between the arrows and route shields. Vertical splices shall not be used on route assemblies with a horizontal dimension of 144 inches or less. The contractor shall not use more than one vertical joint per sign and the joint shall be between route shields.
- Route assemblies that have 36 inch shields and have dimensions greater than 48 inches (both vertical and horizontal) shall have two horizontal splices. One horizontal splice shall be between the cardinal direction and route shields and the other horizontal splice shall be between the arrows and route shields. Vertical splices shall not be used on route assemblies with a horizontal dimension of 144 or less. The contractor shall not use more than one vertical joint per sign and the joint shall be between route shields.
- All Vertical J Assemblies are given a Sign Code of JV
- For JV Assemblies that have a mixture of Interstate and non Interstate shields, arrows and cardinals shall be white on blue.

ROUTE MARKERS & COMPONENTS IN TYPICAL ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matthew R. Rauch
for State Traffic Engineer

DATE 3/18/21

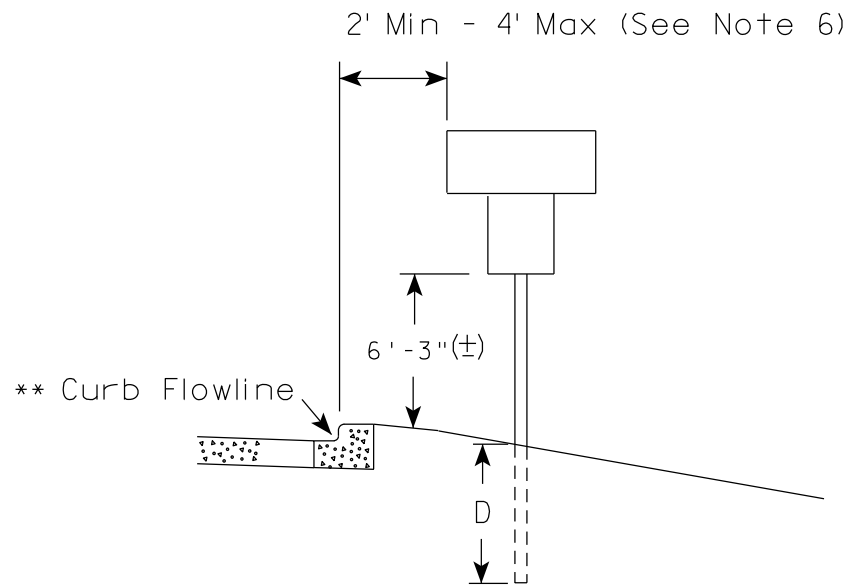
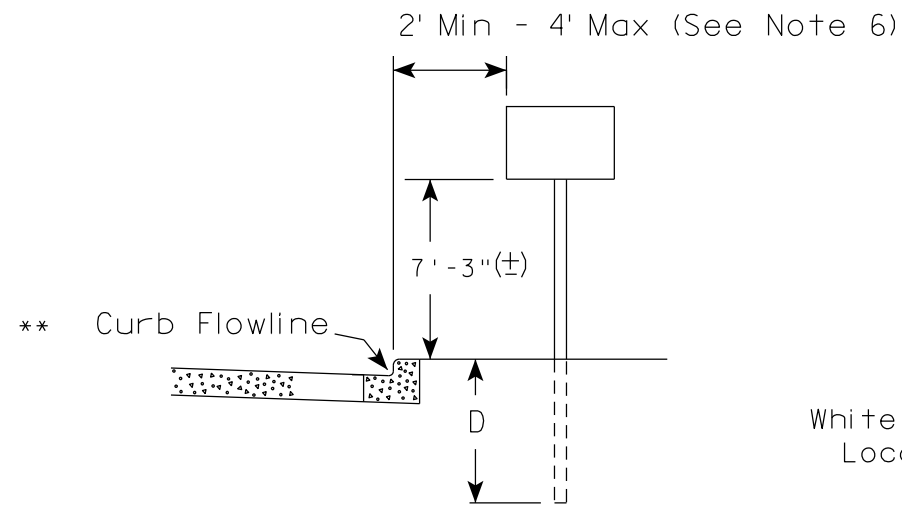
PLATE NO. A2-1S.9

PROJECT NO:

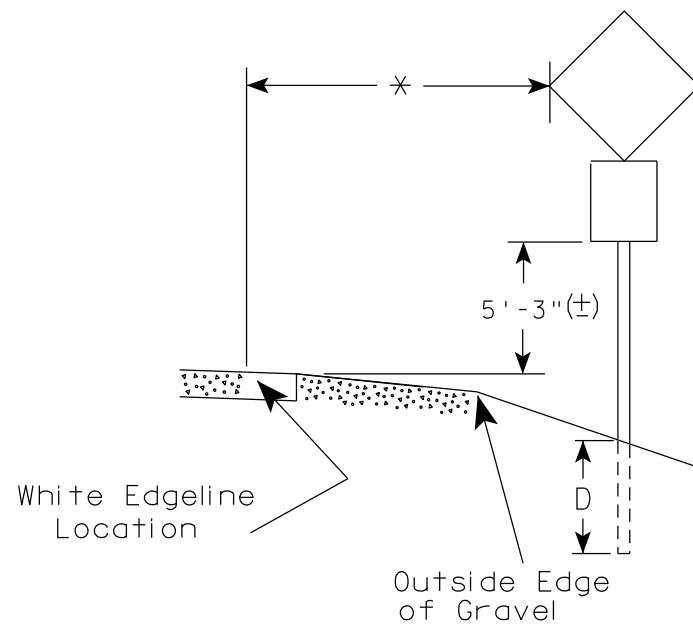
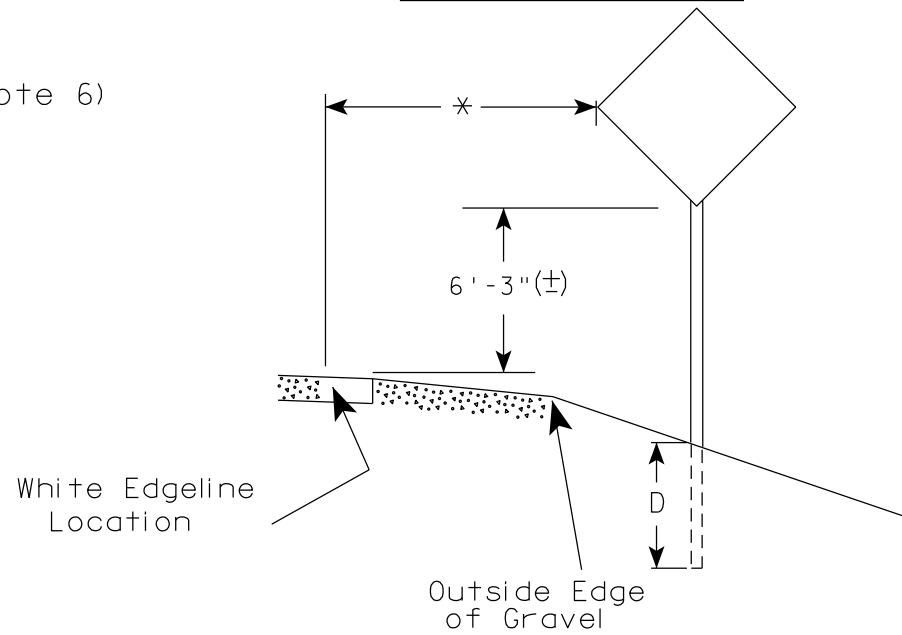
SHEET NO:

E

URBAN AREA



RURAL AREA (See Note 2)



GENERAL NOTES

1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on or behind barrier wall, see A4-10 sign plate.
The Double Arrow sign (W12-1D) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).
3. For expressways and freeways, mounting height is 7'- 3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
4. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±).
5. Offset distance shall be consistent with existing signs or consistent throughout length of project.
6. The (±) tolerance for mounting height is 3 inches.
7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the Engineer.

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

* * The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

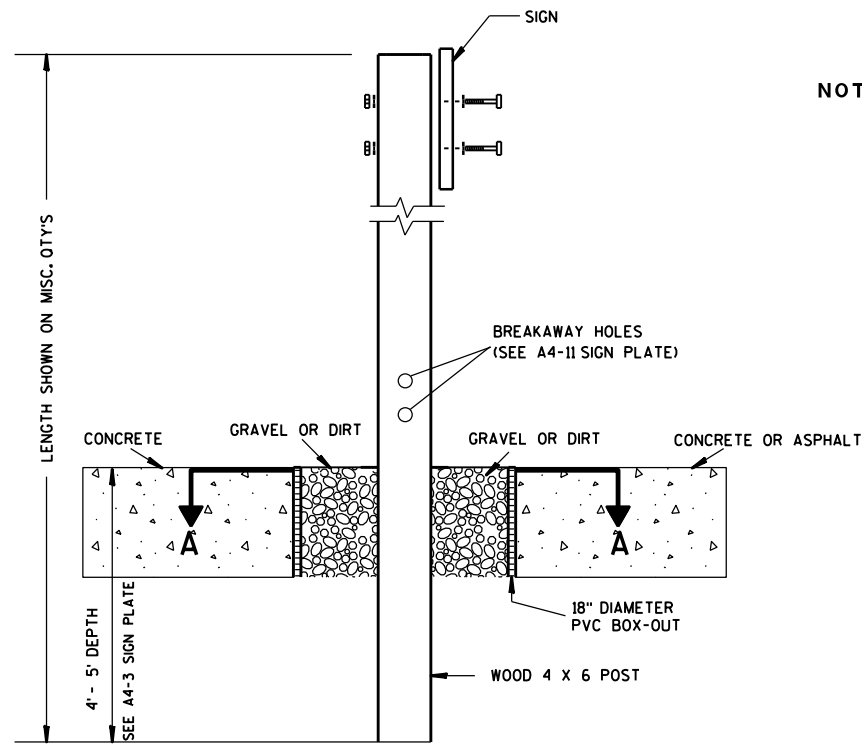
* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

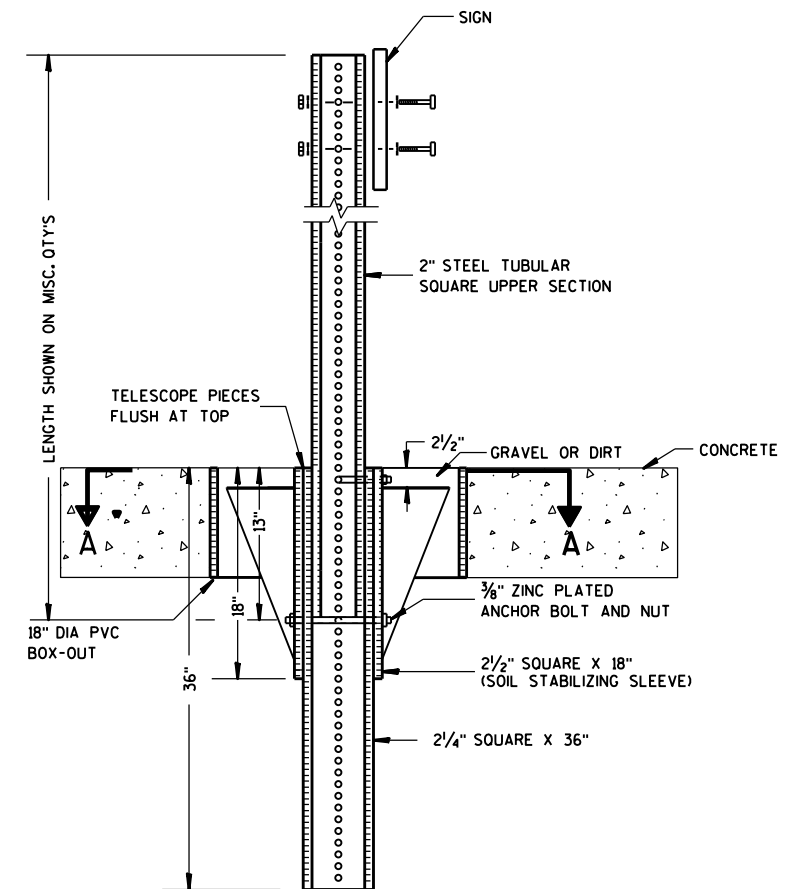
DATE 5/13/2020 PLATE NO. A4-3.22



ELEVATION VIEW

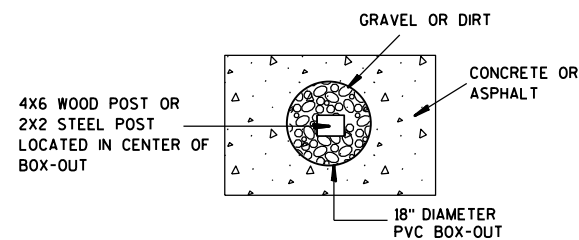
DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

- NOTES:**
1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
 2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
 3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



PLAN VIEW

FOR NEW CONCRETE/ ASPHALT INSTALLATIONS

**SIGN POST
BOX-OUTS
A4-3B**

WISCONSIN DEPT OF TRANSPORTATION

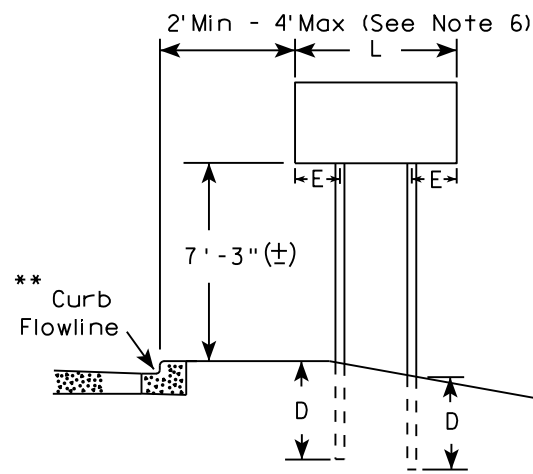
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 1/27/14 PLATE NO. A4-3B.1

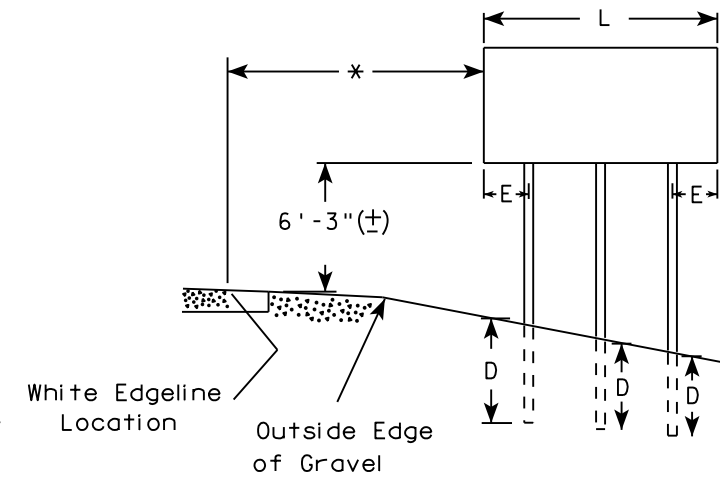
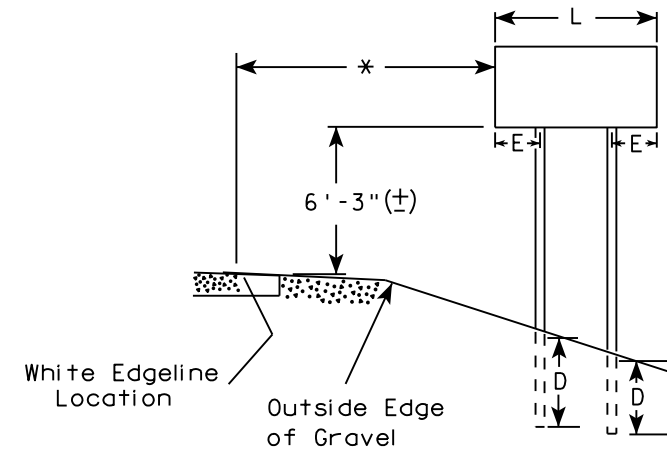
GENERAL NOTES

1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
2. See tables below for required number of posts.
3. For expressways and freeways, mounting height is 7'-3" (±) or 6'-3" (±) depending upon existence of sub-sign.
4. The (±) tolerance for mounting height is 3 inches.
5. J-Assemblies are considered to be one sign for mounting height.
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the engineer.
8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).

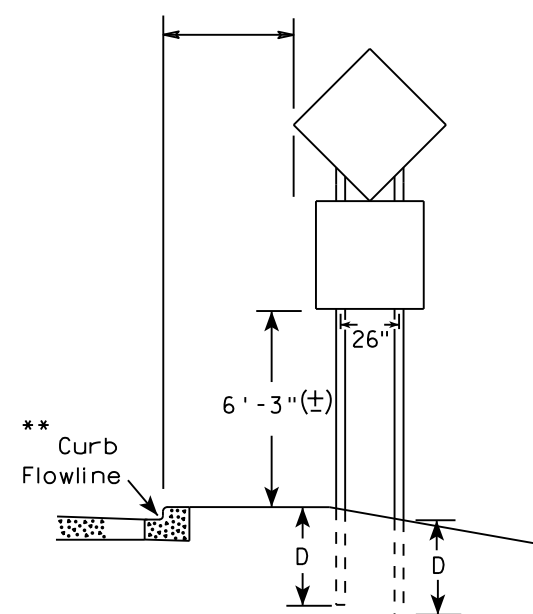
URBAN AREA



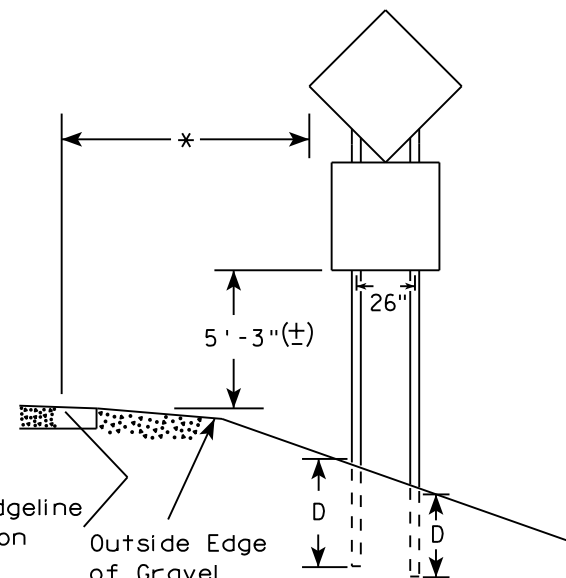
RURAL AREA (See Note 3)



2' Min - 4' Max (See Note 6)



48" DIAMOND WARNING SIGN



48" DIAMOND WARNING SIGN

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

*** See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)	
L	E
Greater than 48" Less than 60"	12"
60" to 108"	L/5

SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)	
L	E
Greater than 108" to 144"	12"

POST EMBEDMENT DEPTH

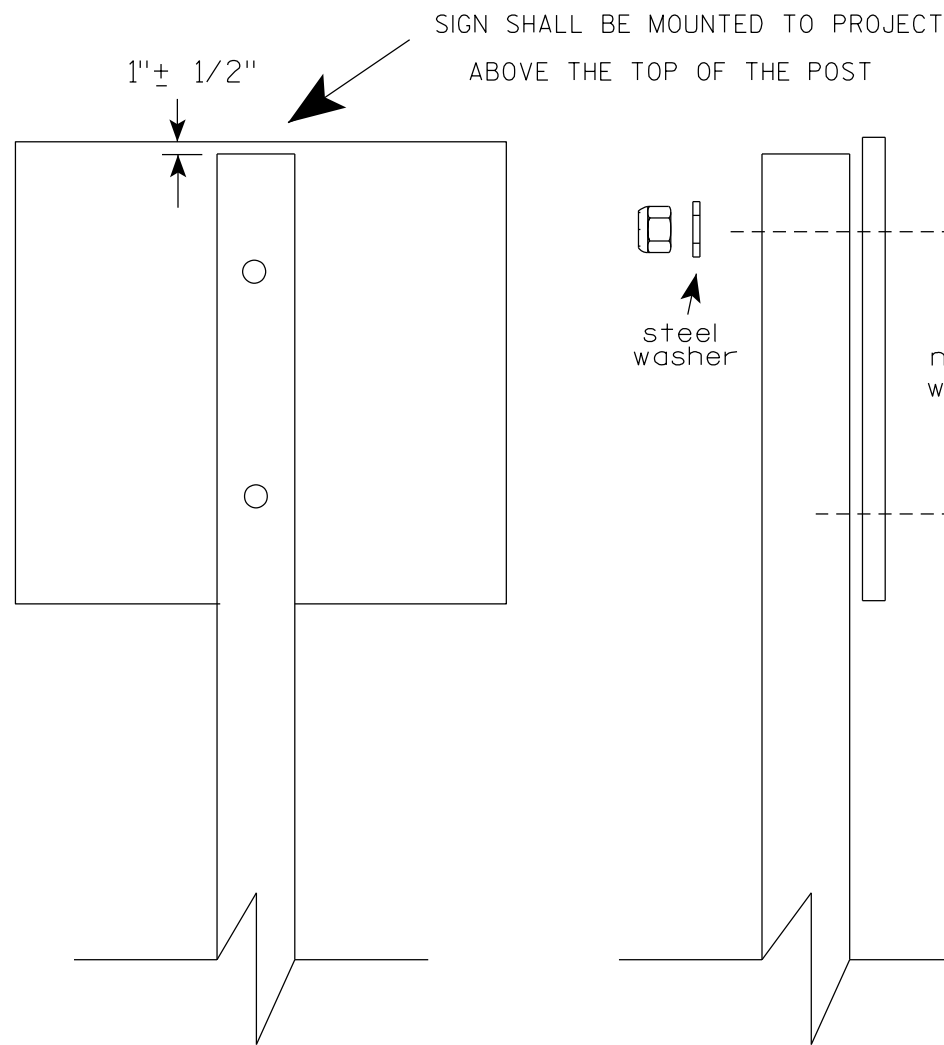
Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 8/21/17 PLATE NO. A4-4.15



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

- Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
- Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

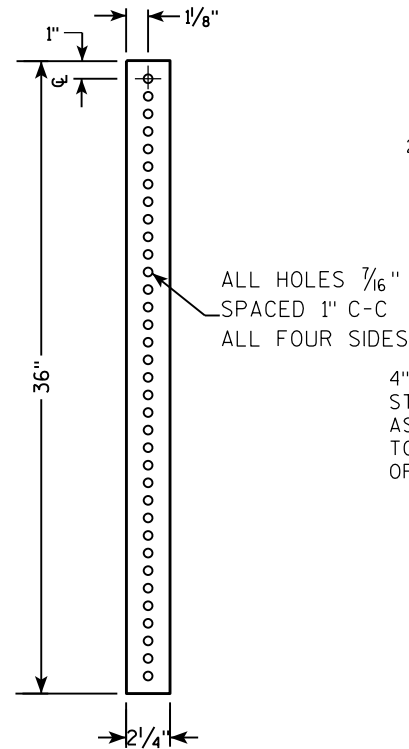
- STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)
- MACHINE BOLTS - 5/16" X 1-3/4" Length w/ lock nuts
- WOOD POSTS (4" x 6")
- LAG SCREWS - 3/8" X 3" (NO STRINGERS ON BACK OF SIGN)
3/8" X 4" (STRINGERS ON BACK OF SIGN)
- SQUARE STEEL POSTS (2" x 2")
- MACHINE BOLTS - 3/8" X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)
3/8" X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)
- RIVETS - 9/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH
- WASHERS (ALL POSTS) -
- 1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
- 1-1/4" O.D. X 3/8" I.D. X .080 NYLON

* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

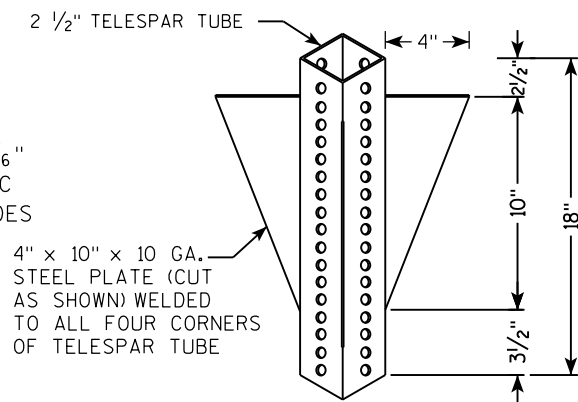
ATTACHMENT OF SIGNS TO POSTS	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R Rauch</i> For State Traffic Engineer
DATE 4/1/2020	PLATE NO. A4-8.9

**TELESCOPIC TUBING ANCHORS
TWO PIECE SYSTEM**

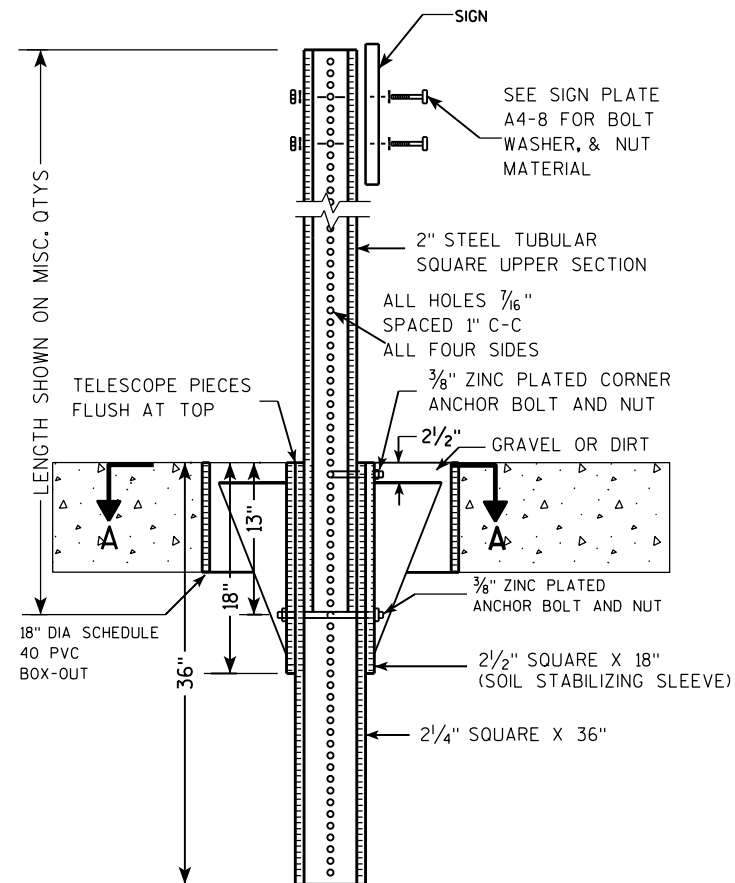
2 1/4" SQUARE
12 GAUGE
PERFORATED
GALVANIZED FINISH



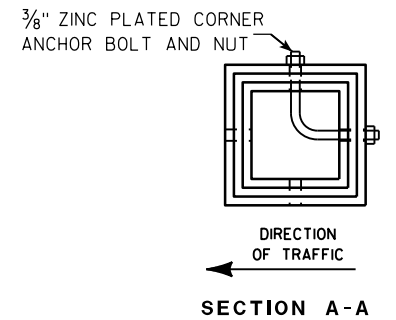
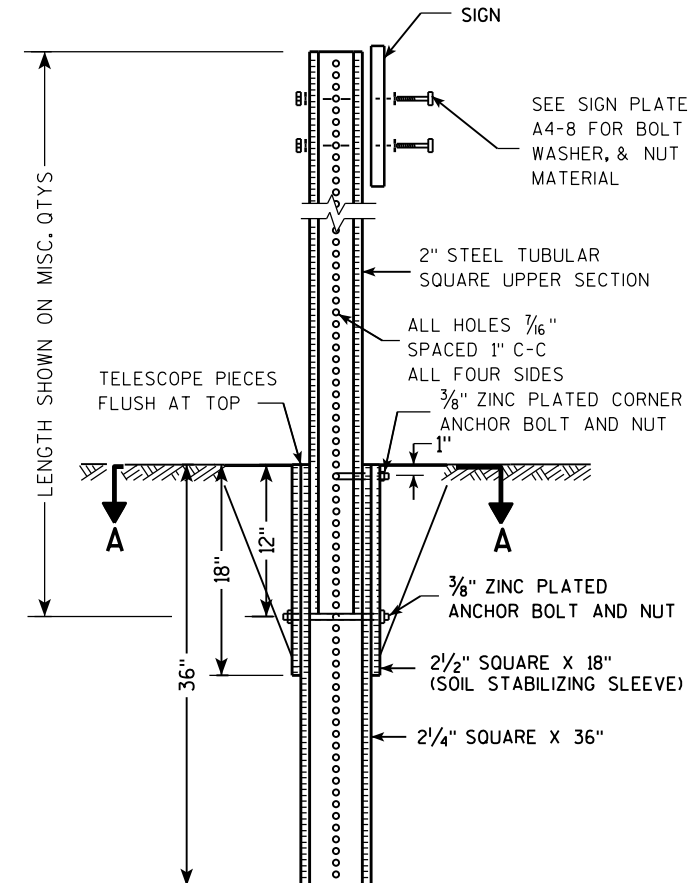
2 1/2" SQUARE
12 GAUGE
OMNI-DIRECTIONAL
PERFORATED
SOIL STABILIZING SLEEVE
GALVANIZED FINISH



**DETAIL OF TUBULAR STEEL SIGN POST
(IN POURED CONCRETE OR ASPHALT)**



**DETAIL OF TUBULAR STEEL SIGN POST
(IN LOCATIONS OTHER THAN POURED CONCRETE OR ASPHALT)**



Area of Sign Installation (Sq. Ft.)	Number of Required Posts
9 or less	1
Greater than 9 less than or equal to 18	2
Greater than 18 less than or equal to 27	3

Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

**TUBULAR STEEL
SIGN POST
A4-9**

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 2/05/15 PLATE NO. A4-9.9

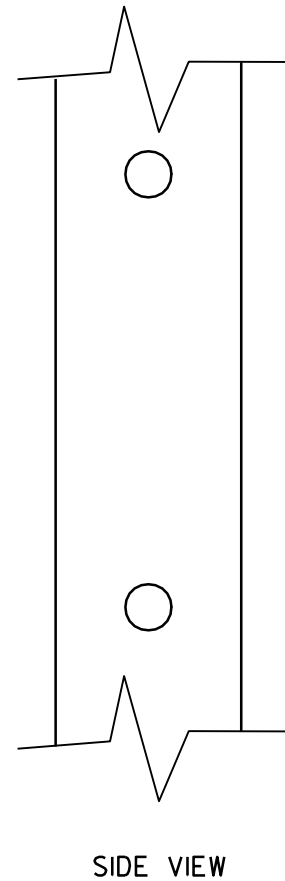
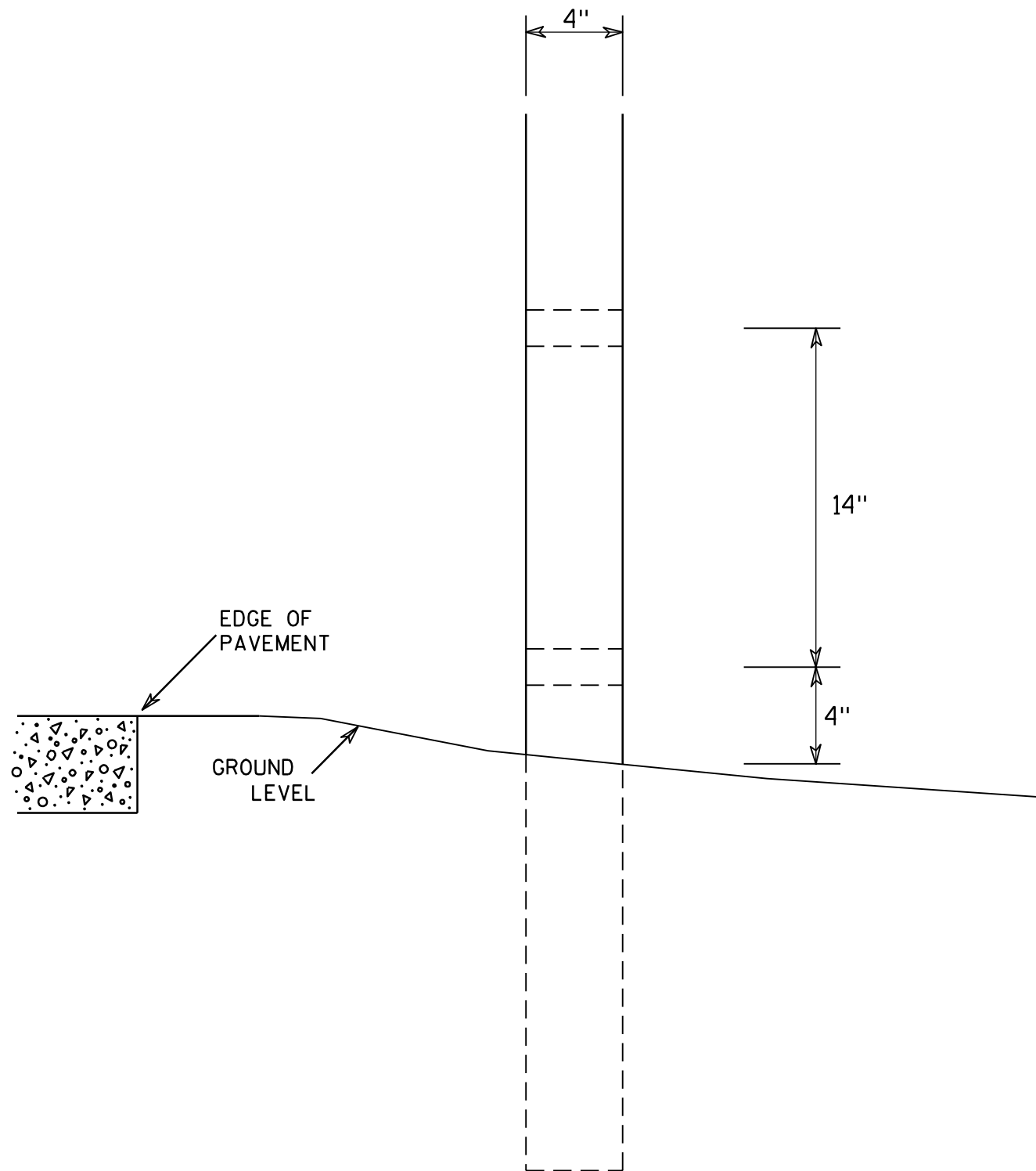
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E




GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

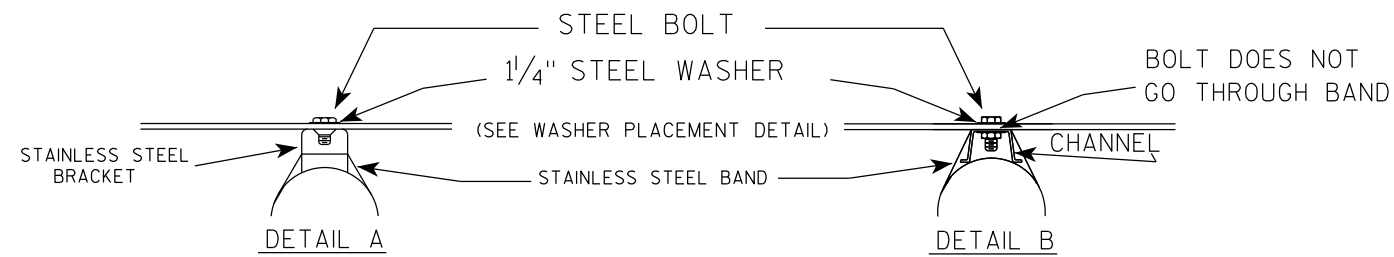
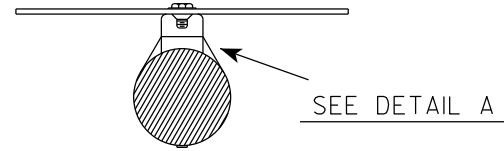
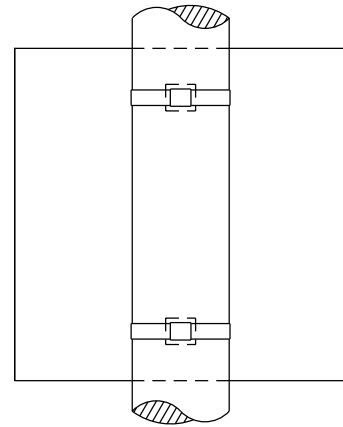
7

7

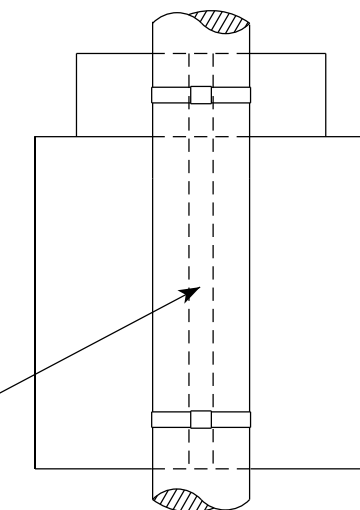
4 X 6 WOOD POST MODIFICATIONS	
<i>WISCONSIN DEPT OF TRANSPORTATION</i>	
APPROVED	 <small>for State Traffic Engineer</small>
DATE <u>3/27/97</u>	PLATE NO. <u>A4-11.2</u>

BANDING

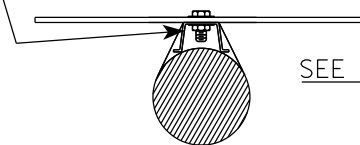
SINGLE SIGN



"J" ASSEMBLY

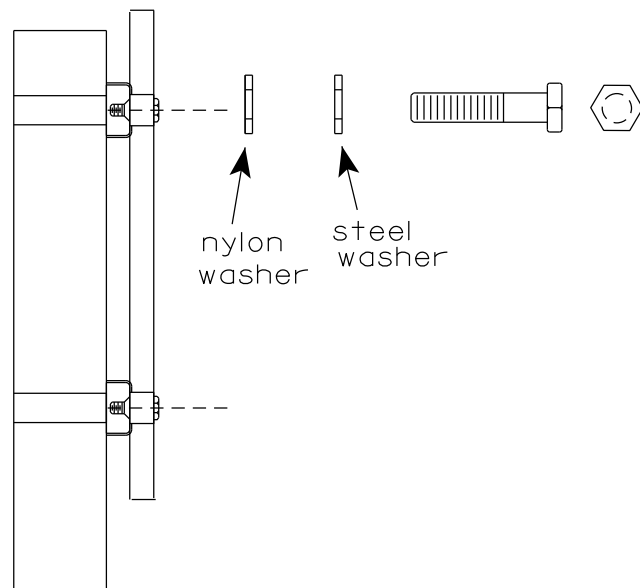


CHANNEL
SEE TYPICAL PANEL
INSTALLATION SHEET



- GENERAL NOTES**
1. Any sign over 3 feet in width shall use the V-Block banding method. See A5-10 standard plate.
 2. Signs 3 feet or greater in height shall have three bracket bands installed. Signs less than 3 feet in height shall have two bracket bands installed.
 3. Banding and assembly bracket shall be stainless steel. All bands shall be $\frac{3}{4}$ " in width and 0.025" thickness.
 4. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
 - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
 - b. Electro-galvanized in accordance with ASTM designation: B 633, Type III, SC 3

WASHER PLACEMENT



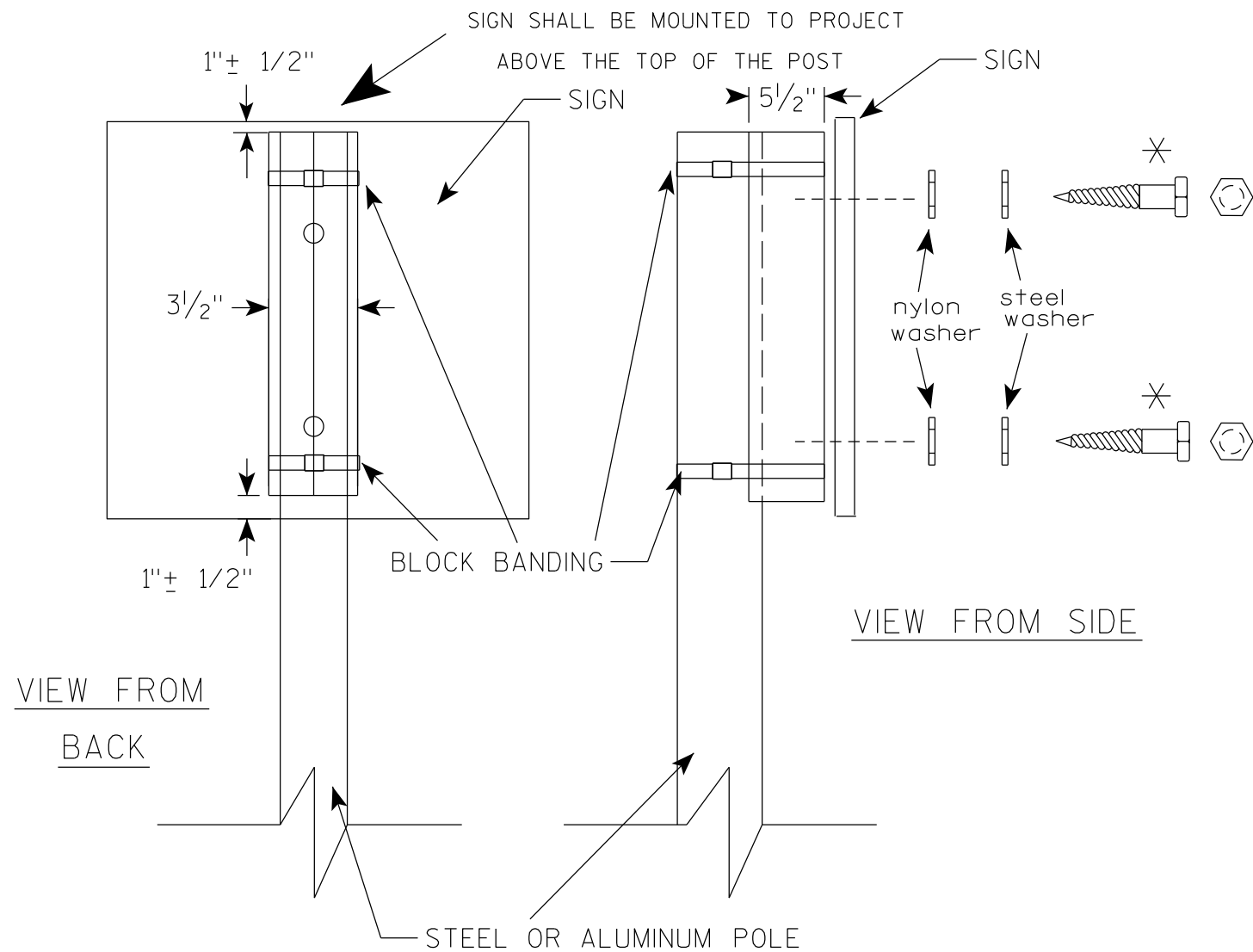
WASHERS (ALL POSTS) -
 1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
 1-1/4" O.D. X 3/8" I.D. X .080 NYLON
 FOR ALL TYPE H SIGNS

STANDARD SIGN
SIGN BANDING DETAILS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

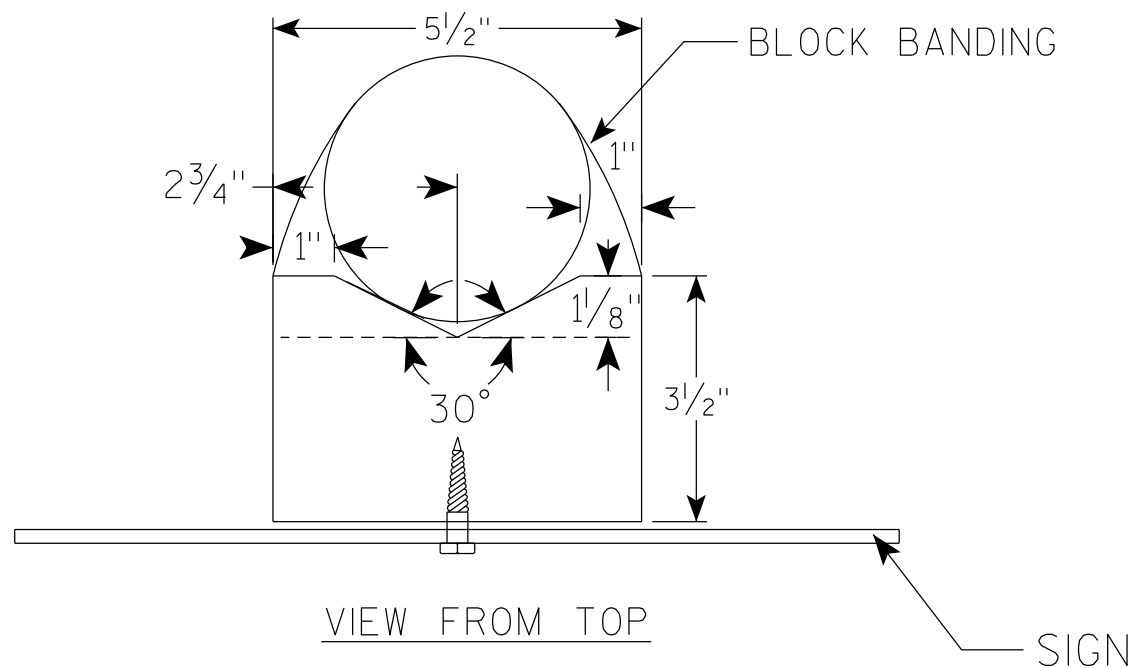
DATE 6/10/19 PLATE NO. A5-9.4



GENERAL NOTES

1. WOOD 4"X6" POST MATERIAL SHALL CONFORM TO 507.2.2 OF THE WisDOT STANDARD SPECIFICATIONS
2. BLOCK BANDING AND CLIPS SHALL BE STAINLESS STEEL, 3/4" WIDTH AND 0.025" THICKNESS
3. SIGNS 3' OR GREATER IN HEIGHT SHALL UTILIZE 3 BLOCK BANDS. SIGNS UNDER 3' IN HEIGHT SHALL UTILIZE 2 BLOCK BANDS
4. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA, BUT NORMALLY THERE ARE TWO. FOR SIGNS GREATER THAN 9 S.F. 3 FASTENERS SHALL BE USED.
5. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
 - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
 - b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3
6. ALL BOLTS SHALL HAVE HEXAGONAL HEADS.
7. STEEL WASHERS SHALL BE 1 1/4" O.D. X 3/8" I.D. X 1/16"
8. NYLON WASHERS SHALL BE 1 1/4" O.D. X 3/8" I.D. X .080 FOR TYPE H OR TYPE F FACE SIGN

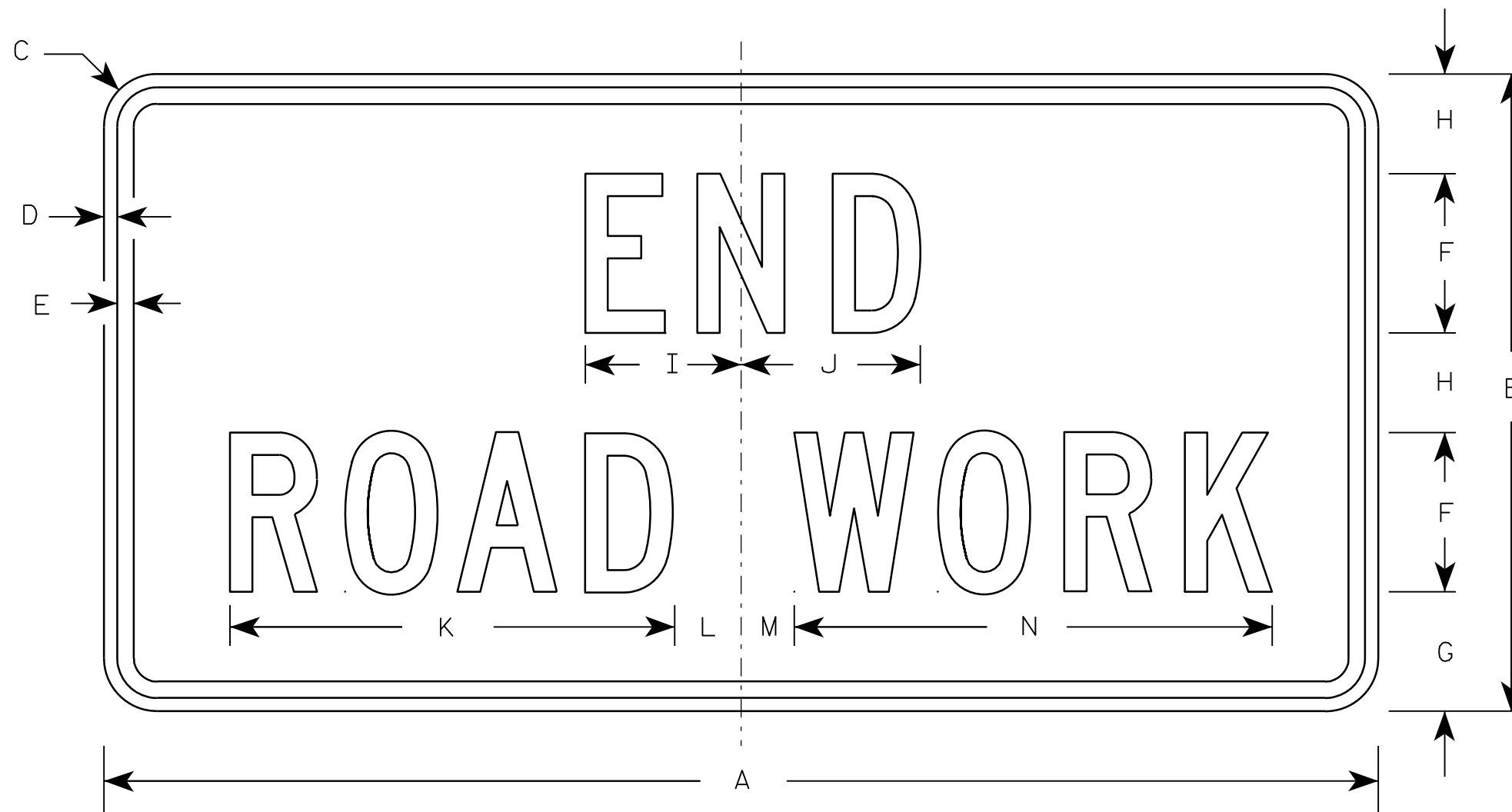
* LAG BOLTS SHALL BE 3/8" X 2 1/2"



BLOCK BANDING DETAIL (V-BLOCK OPTION)	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R Rauch</i> for State Traffic Engineer
DATE 6/10/19	PLATE NO. A5-10.2

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



G20-2A

Metric equivalent
for this sign is:

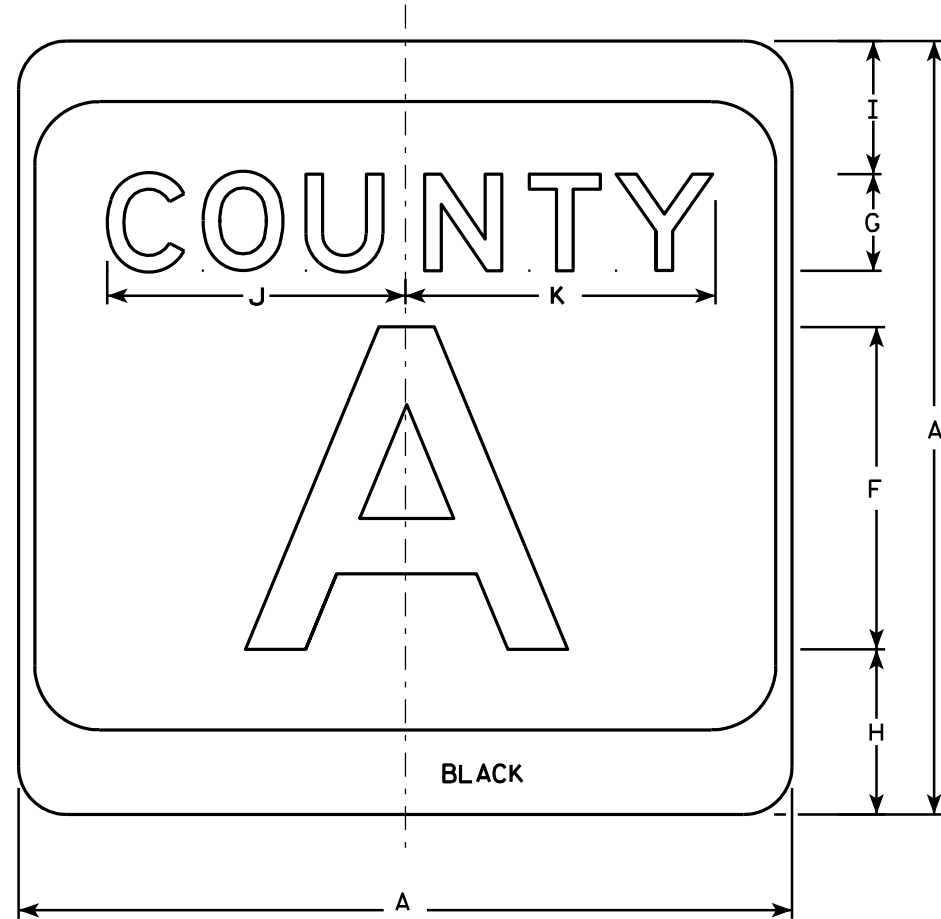
SIZE	
1	900 mm X 450 mm
2	1200 mm X 600 mm
3	1200 mm X 600 mm
4	1200 mm X 600 mm
5	1200 mm X 600 mm

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.	Area sq.
1	36	18	1 1/8	3/8	1/2	4	3 3/4	2 1/2	4 1/8	4 1/8	11 1/8	2	1	12 1/8													4.5	0.41
2	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0	0.72
3	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0	0.72
4	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0	0.72
5	48	24	1 1/2	1/2	5/8	6	4 1/2	3 3/4	5 7/8	6 3/4	16 3/4	2 1/2	1 3/4	18 1/2													8.0	0.72

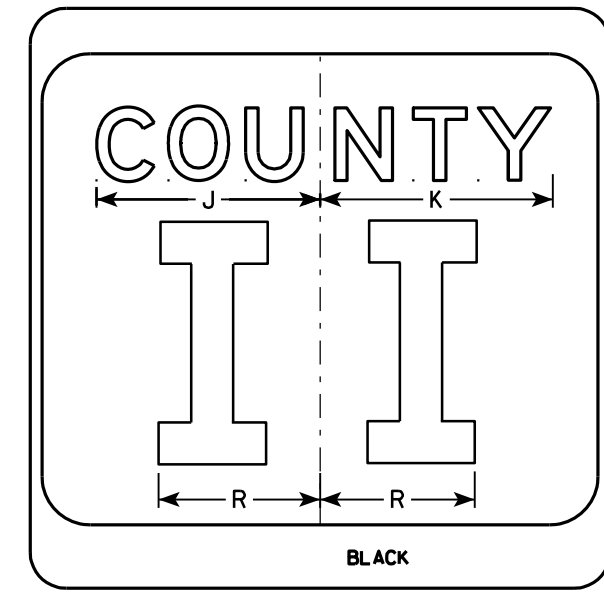
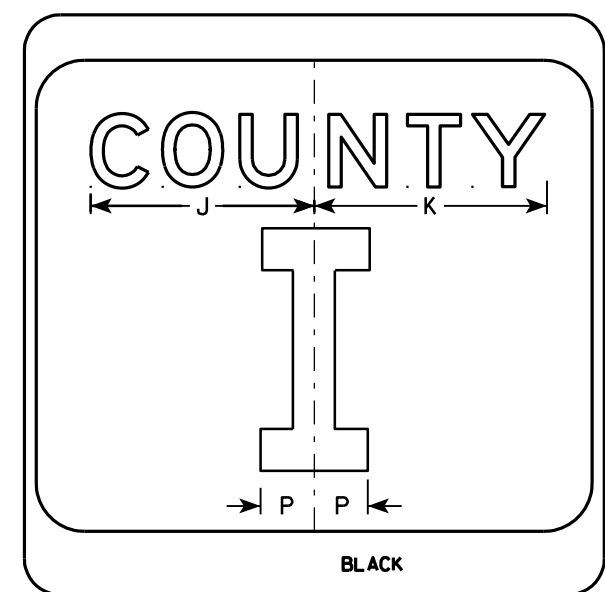
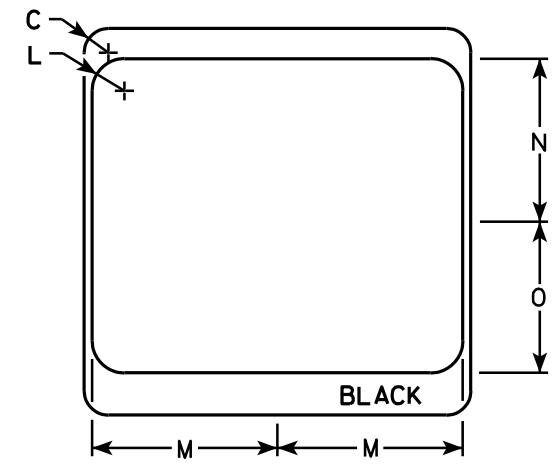
STANDARD SIGN G20-2A	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R. Rauch</i> For State Traffic Engineer
DATE 9/30/09	PLATE NO. G20-2A.8

NOTES

1. Sign is Type II - see Note 7 - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White & Black - See Note 7
Message - Black
3. Message Series - see Note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Message Series E for 1 letter.
Message Series D for 2 letters unless message is too big then Series C.
Message Series C for 3 letters unless message is too big then Series B.
6. Substitute appropriate letters & optically center to achieve proper balance.
7. Permanent Signs
Background - Type H Reflective
Detour or temporary Signs
Background - Reflective



M1-5A



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24		1 1/2			10	3	5 1/8	4 1/8	9 1/4	9 5/8	2	11 1/2	10 1/8	9 3/8	2 1/4		6 5/8									4.0
3	36		2 1/4			16	4	7 5/8	5 5/8	12 1/4	12 7/8	3	17 1/8	15 1/4	14	3 3/8		10									9.0
4	36		2 1/4			16	4	7 5/8	5 5/8	12 1/4	12 7/8	3	17 1/8	15 1/4	14	3 3/8		10									9.0
5	36		2 1/4			16	4	7 5/8	5 5/8	12 1/4	12 7/8	3	17 1/8	15 1/4	14	3 3/8		10									9.0

CTH MARKER
M1-5A FOR ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

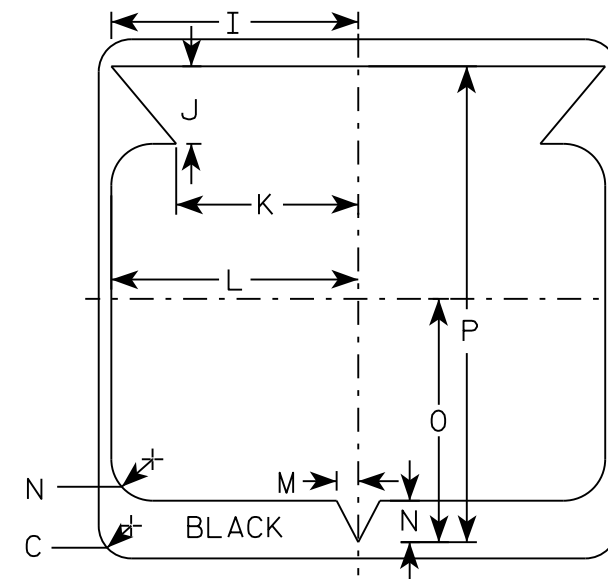
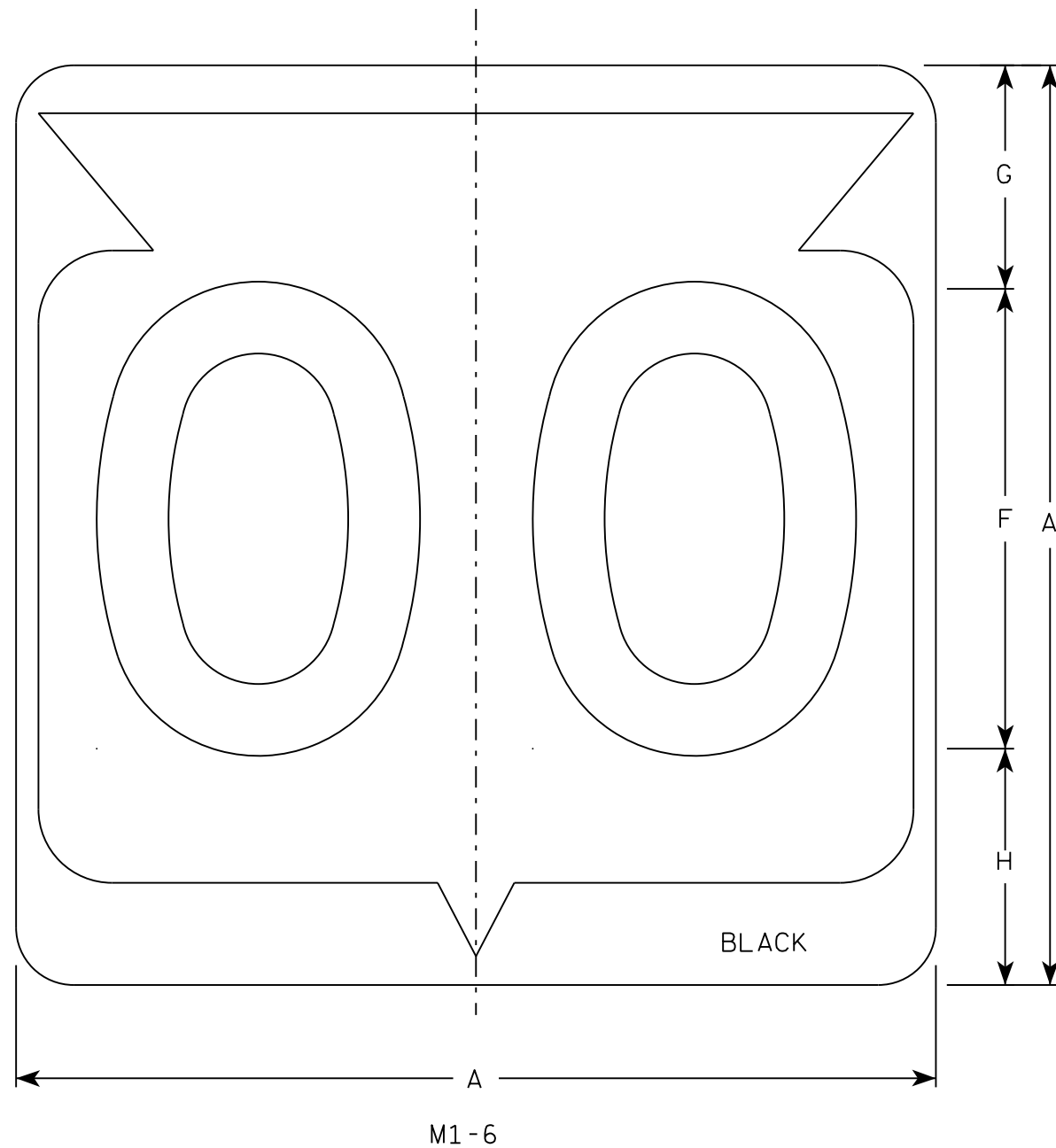
APPROVED *Matthew R. Raub*
For State Traffic Engineer

DATE 9/27/11 PLATE NO. MI-5A.8

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - D except 3 number signs Series C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24		1 1/2			12	5 1/2	6 1/2	10 1/4	2 1/2	8 7/8	11 1/2	1	1 7/8	11 1/4	21 7/8											4.0
3	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0
4	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0
5	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0

STATE ROUTE MARKER
M1-6 FOR ASSEMBLIES

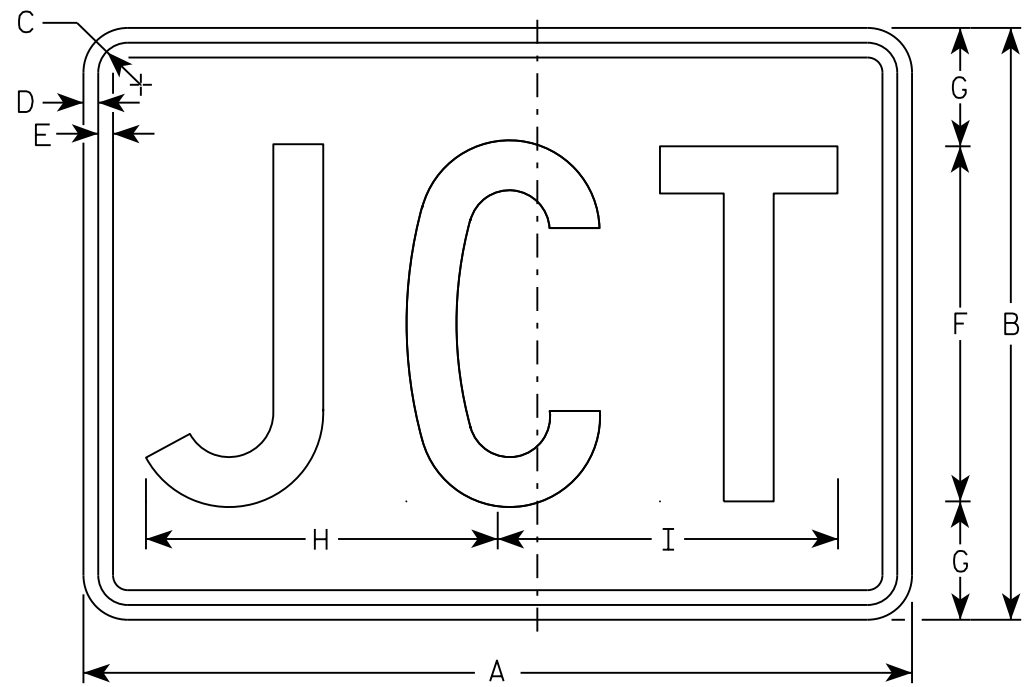
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

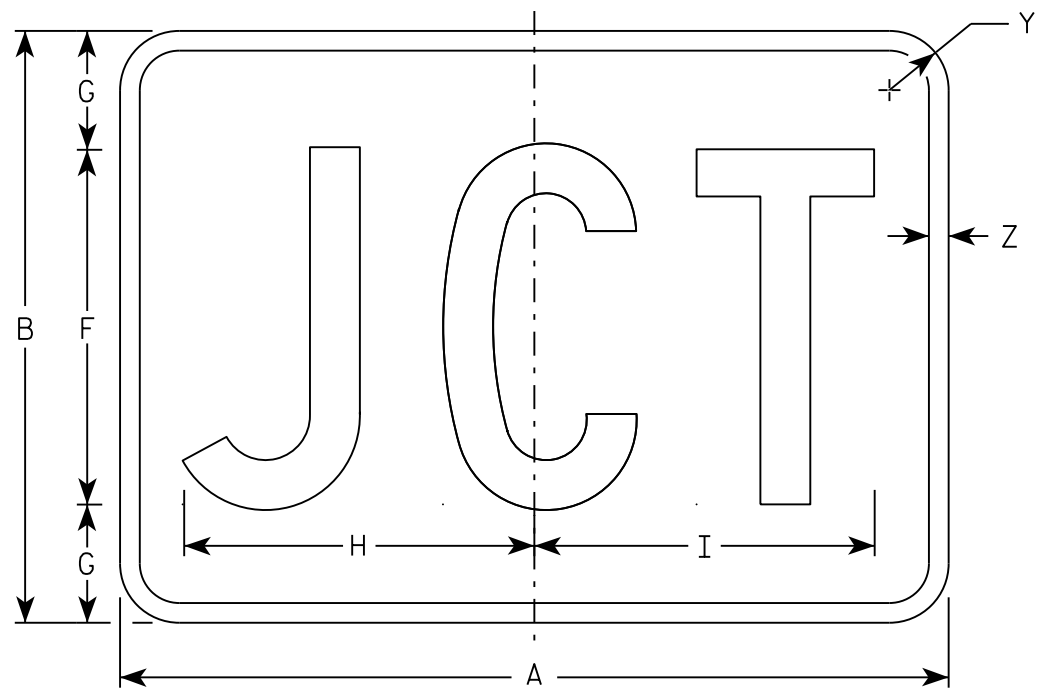
DATE 3/16/18 PLATE NO. M1-6.10

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ **E**

7



M2-1
MM2-1
MP2-1



MB2-1
MK2-1
MN2-1
MR2-1

NOTES

1. Sign is Type II - Type H
2. Color:
Background - See note 5
Message - See note 5
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. M2-1 Background - White
Message - Black
MB2-1 Background - Blue
Message - White
MK2-1 Background - Green
Message - White
MM2-1 Background - White
Message - Green
MN2-1 Background - Brown
Message - White
MP2-1 Background - White
Message - Blue
MR2-1 Background - Brown
Message - Yellow

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21	15	1 1/8	3/8	3/8	9	3	8 7/8	8 5/8																1 1/2	1/2	2.20
3	30	21	1 1/8	3/8	3/8	13	4	12 7/8	12 3/8																1 1/2	1/2	4.40
4	30	21	1 1/8	3/8	3/8	13	4	12 7/8	12 3/8																1 1/2	1/2	4.40
5	30	21	1 1/8	3/8	3/8	13	4	12 7/8	12 3/8																1 1/2	1/2	4.40

STANDARD SIGN
M2-1

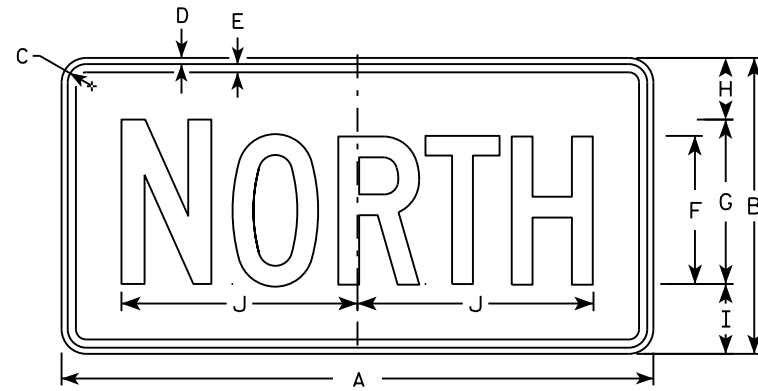
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

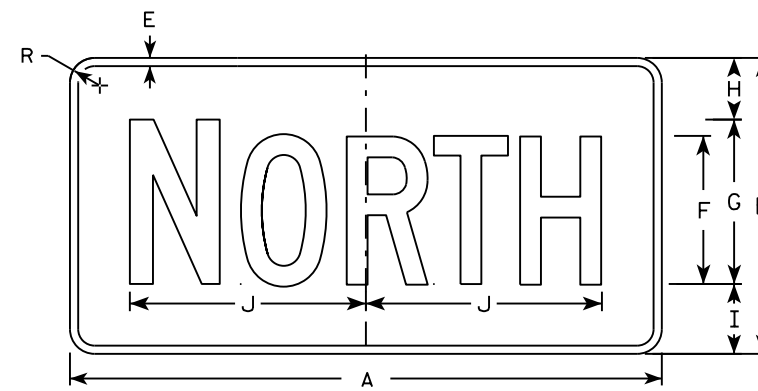
DATE 10/15/15 PLATE NO. M2-1.12

NOTES

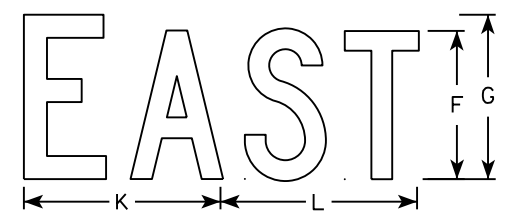
- All Signs Type II - Type H
- Color:
 - Background - See note 5
 - Message - See note 5
- Message Series - C
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- | | |
|------------------|--------------------|
| M3-1 thru M3-4 | Background - White |
| | Message - Black |
| MB3-1 thru MB3-4 | Background - Blue |
| | Message - White |
| MK3-1 thru MK3-4 | Background - Green |
| | Message - White |
| MM3-1 thru MM3-4 | Background - White |
| | Message - Green |
| MN3-1 thru MN3-4 | Background - Brown |
| | Message - White |
| MP3-1 thru MP3-4 | Background - White |
| | Message - Blue |
- Note the first letter of each direction is larger than the remainder of the message.



M3-1
MM3-1
MP3-1



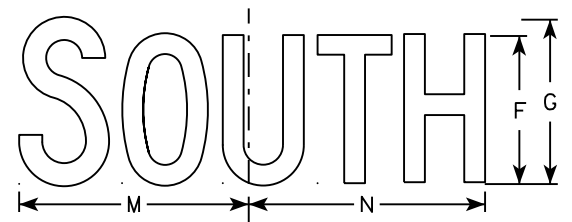
MB3-1
MK3-1
MN3-1



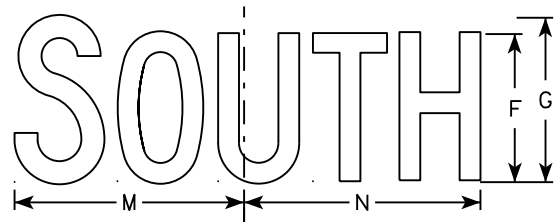
M3-2
MM3-2
MP3-2



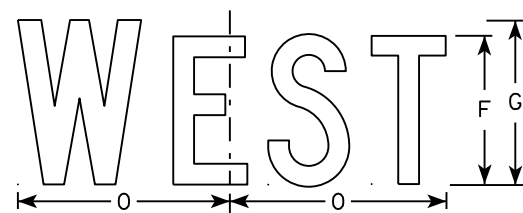
MB3-2
MK3-2
MN3-2



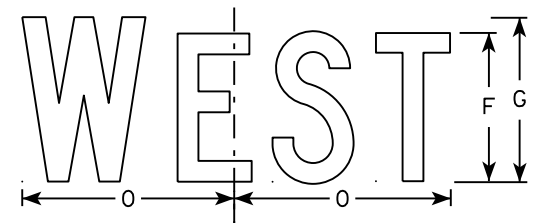
M3-3
MM3-3
MP3-3



MB3-3
MK3-3
MN3-3



M3-4
MM3-4
MP3-4



MB3-4
MK3-4
MN3-4

7

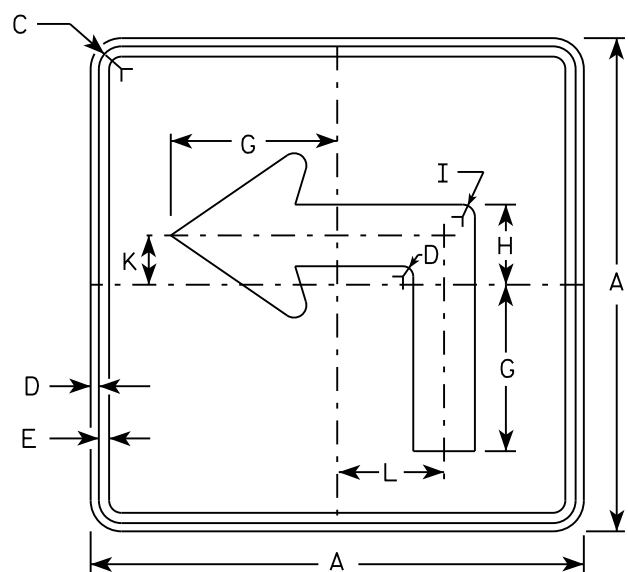
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	12	1 1/8	3/8	3/8	6	7	2 1/4	2 3/4	10 1/4	7 7/8	8 3/8	10 1/4	9 3/4	8 3/4			1 1/2									2.00
3	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
4	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
5	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5

STANDARD SIGNS
M3-1 thru M3-4
SERIES

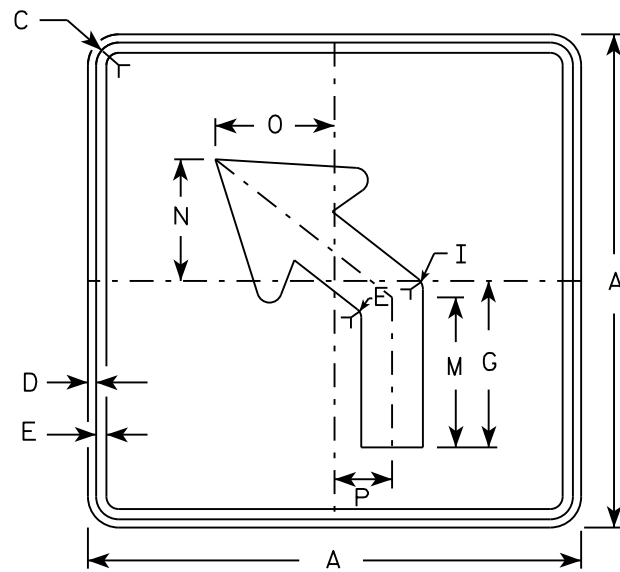
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

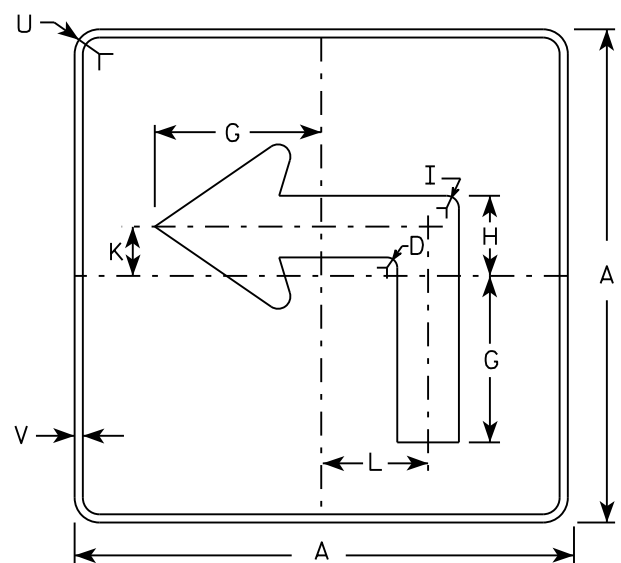
DATE 10/15/15 PLATE NO. M3-1.14



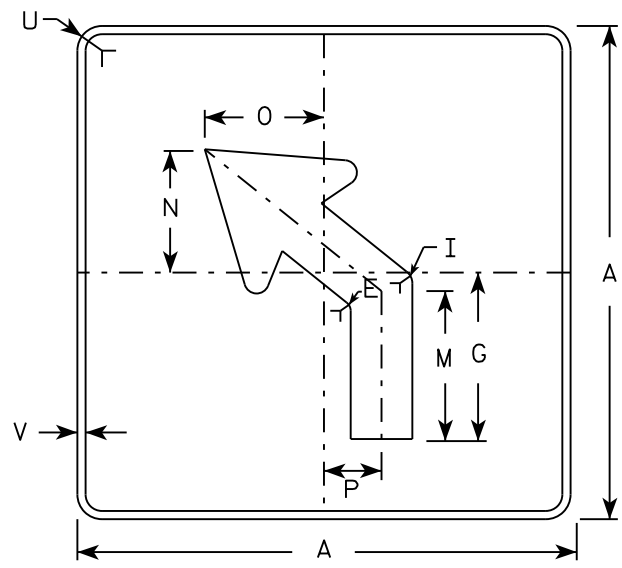
M5-1L
MM5-1L
M05-1L
MP5-1L



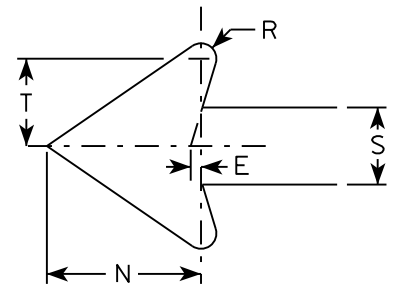
M5-2L
MM5-2L
M05-2L
MP5-2L



MB5-1L
MK5-1L
MN5-1L
MR5-1L



MB5-2L
MK5-2L
MN5-2L
MR5-2L



NOTES

- Signs are Type II - Type H reflective except as shown
- Color:
 - Background - See note 4
 - Message - See note 4
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- M5-1 and M5-2 Background - White
Message - Black
 - MB5-1 and MB5-2 Background - Blue
Message - White
 - MK5-1 and MK5-2 Background - Green
Message - White
 - MM5-1 and MM5-2 Background - White
Message - Green
 - MN5-1 and MN5-2 Background - Brown
Message - White
 - M05-1 and M05-2 Background - Orange - Type F Reflective
Message - Black
 - MP5-1 and MP5-2 Background - White - Type H Reflective
Message - Blue
 - MR5-1 and MR5-2 Background - Brown
Message - Yellow
- M5-1R same as M5-1L except arrow points right.
- M5-2R same as M5-2L except arrow tilts right.

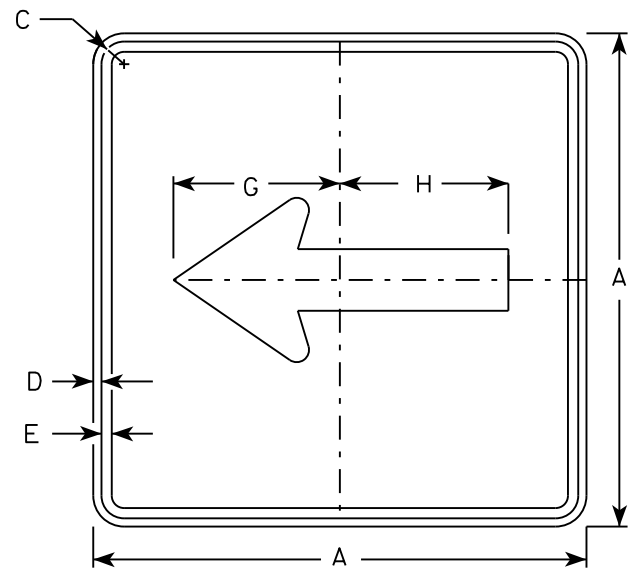
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7	3 3/8	5/8		2 1/8	4 1/2	6 3/8	5 1/4	5	2 1/2		1/2	2 5/8	3	1 1/2	1/2					3.06
3	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25
4	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25
5	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25

STANDARD SIGN
M5-1 & M5-2

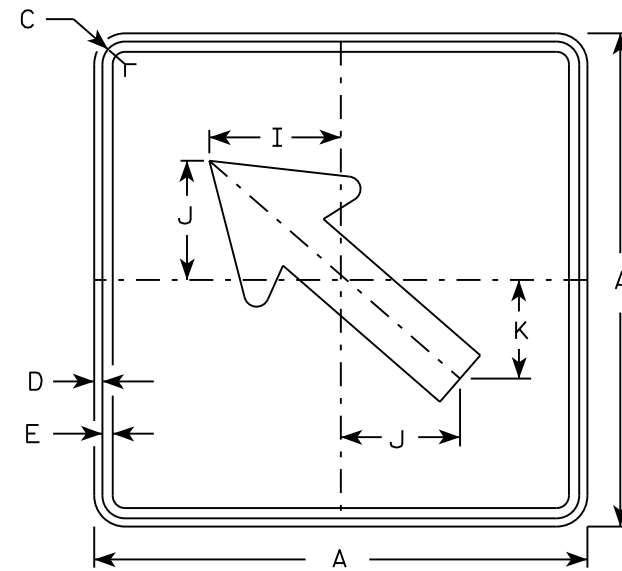
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

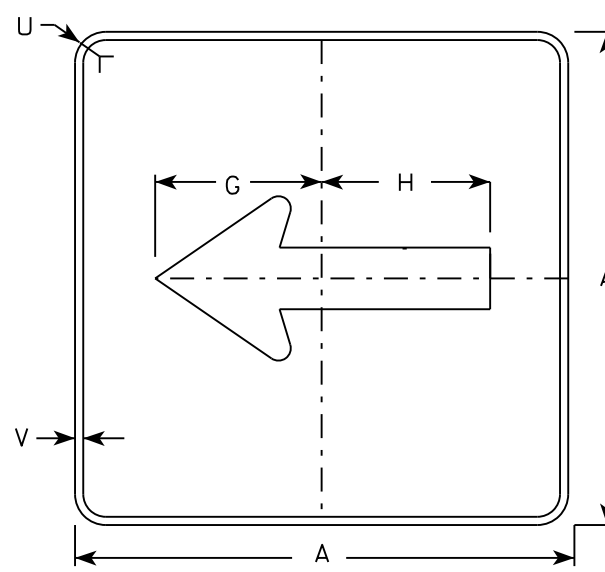
DATE 10/15/15 PLATE NO. M5-1.13



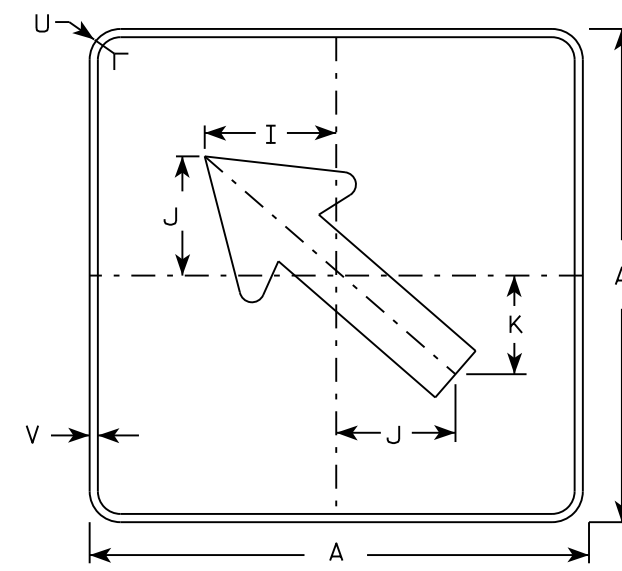
M6-1
MM6-1
M06-1
MP6-1



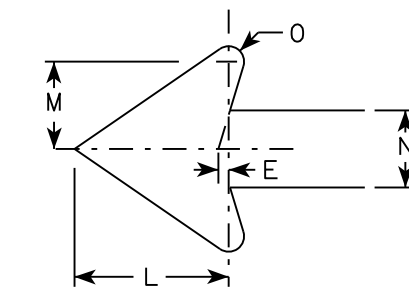
M6-2
MM6-2
M06-2
MP6-2



MB6-1
MK6-1
MN6-1
MR6-1



MB6-2
MK6-2
MN6-2
MR6-2



NOTES

- Signs are Type II - Type H except as Shown
- Color:
Background - See note 4
Message - See note 4
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- M6-1 and M6-2 Background - White
Message - Black
MB6-1 and MB6-2 Background - Blue
Message - White
MK6-1 and MK6-2 Background - Green
Message - White
MM6-1 and MM6-2 Background - White
Message - Green
MN6-1 and MN6-2 Background - Brown
Message - White
M06-1 and M06-2 Background - Orange - Type F Reflective
Message - Black
MP6-1 and MP6-2 Background - White
Message - Blue
MR6-1 and MR6-2 Background - Brown
Message - Yellow

7

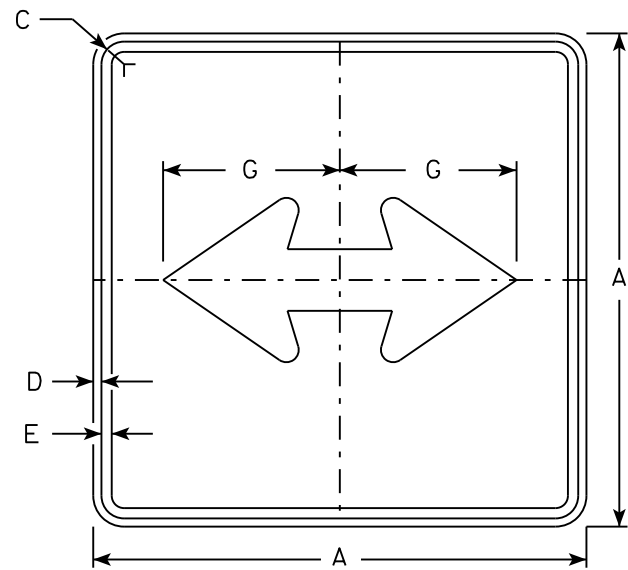
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7 1/2	7 1/8	5 5/8	5	4 1/4	5 1/4	3	2 5/8	1/2						1 1/2	1/2					3.06
3	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25
4	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25
5	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25

STANDARD SIGN
M6-1 & M6-2
SERIES

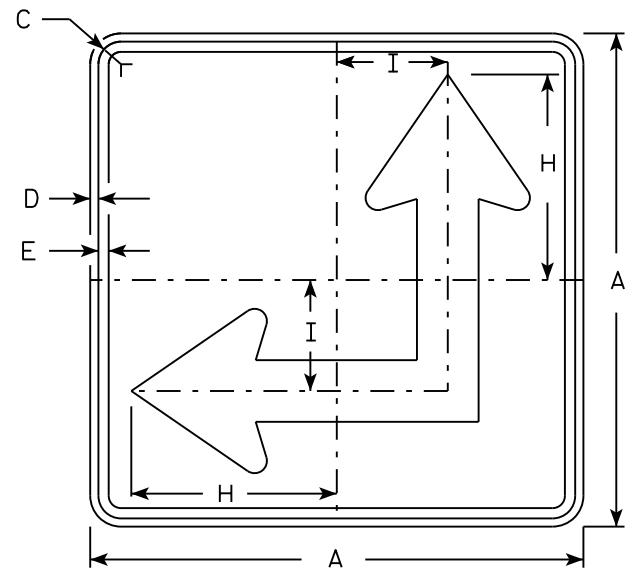
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

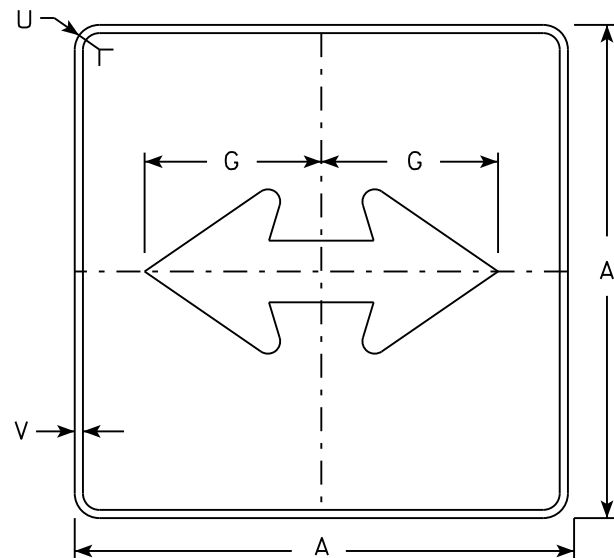
DATE 10/15/15 PLATE NO. M6-1.15



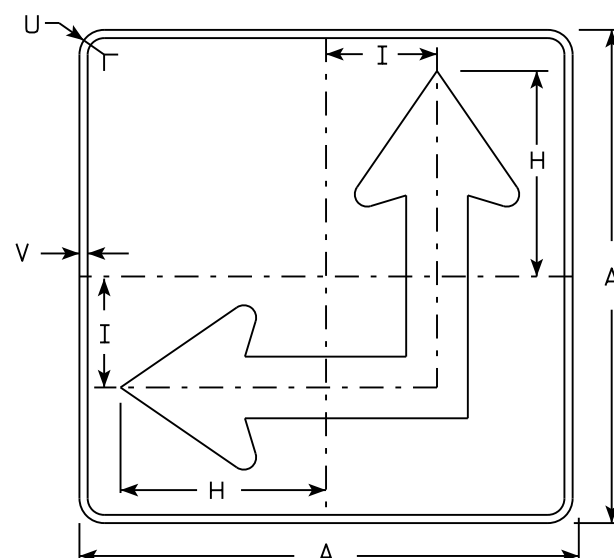
M6-4
MM6-4
M06-4
MP6-4



M6-6
MM6-6
M06-6
MP6-6



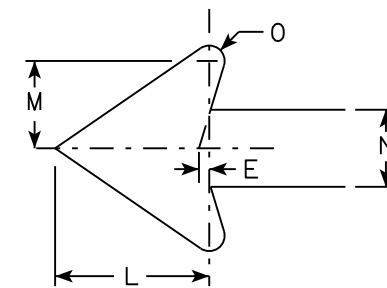
MB6-4
MK6-4
MN6-4
MR6-4



MB6-6
MK6-6
MN6-6
MR6-6

NOTES

- Signs are Type II - Type H except as Shown
- Color:
Background - See Note 4
Message - See Note 4
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- M6-4 and M6-6 Background - White
Message - Black
MB6-4 and MB6-6 Background - Blue
Message - White
MK6-4 and MK6-6 Background - Green
Message - White
MM6-4 and MM6-6 Background - White
Message - Green
MN6-4 and MN6-6 Background - Brown
Message - White
M06-4 and M06-6 Background - Orange - Type F Reflective
Message - Black
MP6-4 and MP6-6 Background - White
Message - Blue
MR6-4 and MR6-6 Background - Brown
Message - Yellow
- M6-6R same as M6-6L except arrow points ahead and right.



7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7 1/2	8 3/4	4 1/4			5 1/4	3	2 5/8	1/2						1 1/2	1/2					3.06
3	30		1 3/8	1/2	5/8		10 3/4	12 1/2	6 3/4			7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25
4	30		1 3/8	1/2	5/8		10 3/4	12 1/2	6 3/4			7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25
5	30		1 3/8	1/2	5/8		10 3/4	12 1/2	6 3/4			7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25

STANDARD SIGN
M6-4 & M6-6
SERIES

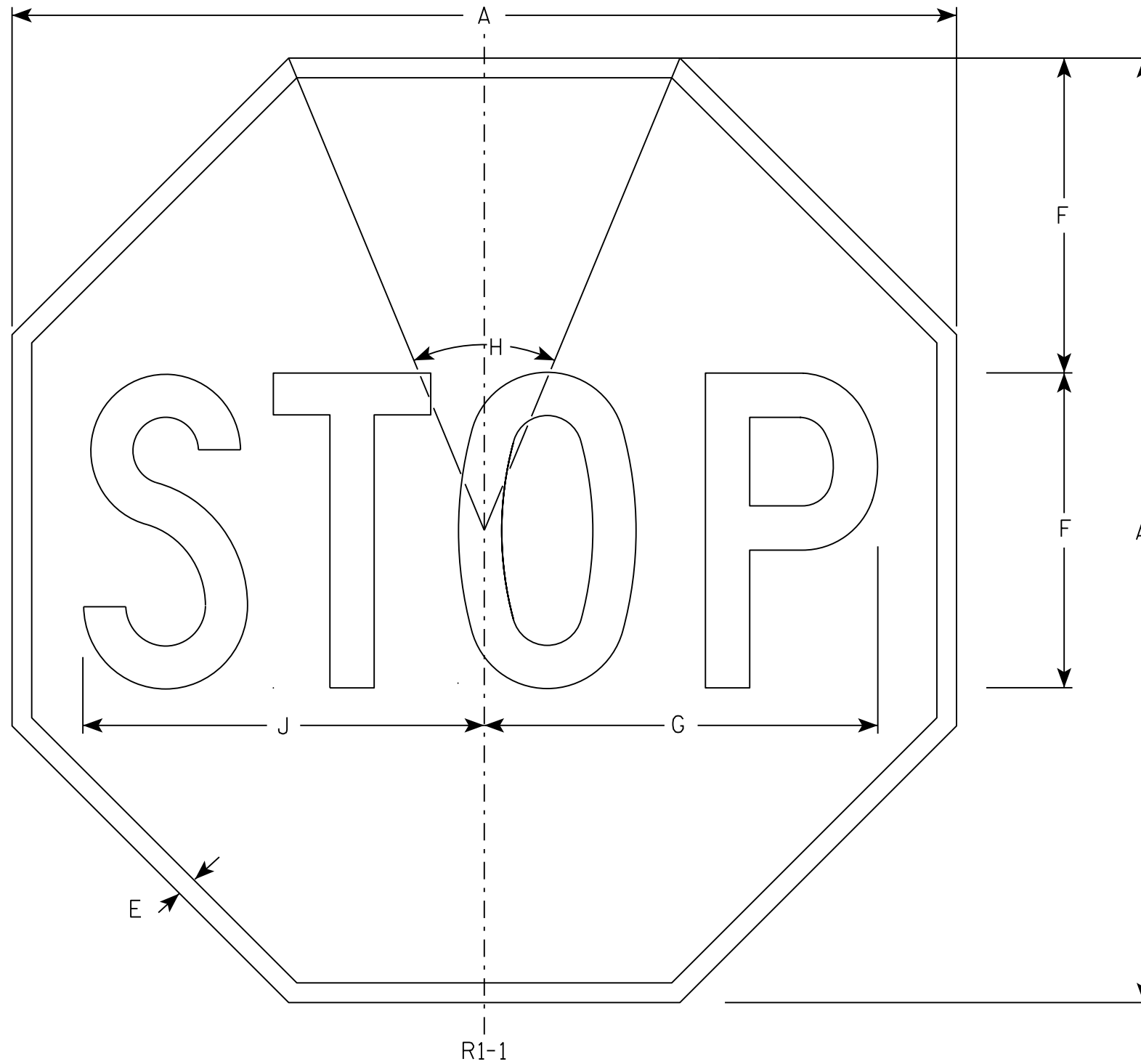
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 10/15/15 PLATE NO. M6-4.10

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Red
Message - White
3. Message Series - C



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30				5/8	10	12 1/2	45°		12 3/4																	5.18
2S	30				5/8	10	12 1/2	45°		12 3/4																	5.18
2M	36				3/4	12	15	45°		15 3/8																	7.46
3	36				3/4	12	15	45°		15 3/8																	7.46
4	48				1	16	20	45°		20 1/2																	13.25
5	48				1	16	20	45°		20 1/2																	13.25
6	18				3/8	6	7 3/4	45°		7 3/4																	1.86
7	12				1/4	4	5	45°		5 1/8																	0.78

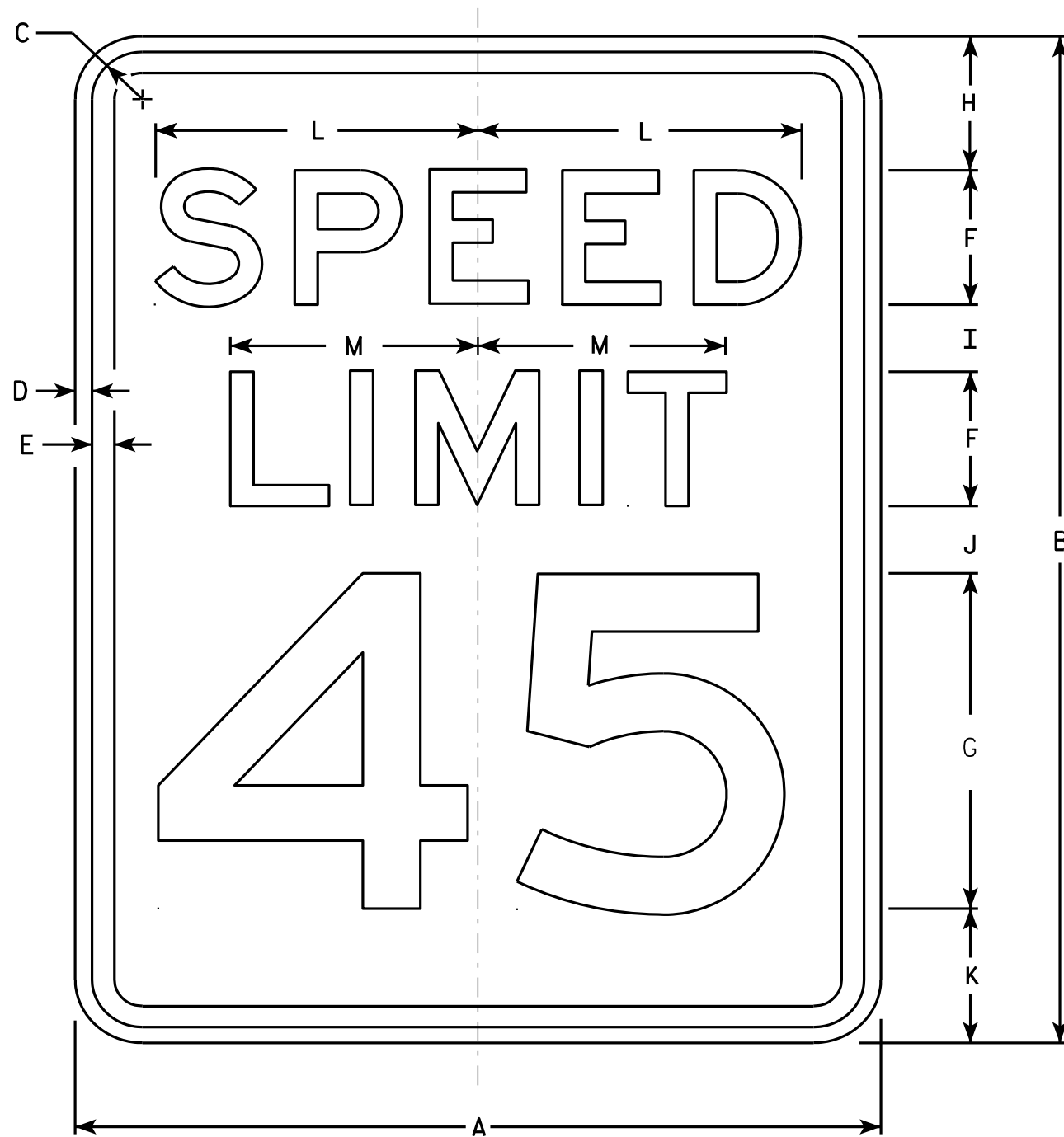
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

STANDARD SIGN
R1-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 11/12/15 PLATE NO. R1-1.13



R2-1

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - Black
3. Message Series - E
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals and optically adjust spacing to achieve proper balance.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18	24	1 1/8	3/8	1/2	3	8	3	2	2	3	7 1/4	5 1/2														3.0
2S	24	30	1 1/8	3/8	1/2	4	10	3	2 1/4	3 3/8	3 3/8	9 5/8	7 3/8														5.0
2M	30	36	1 3/8	1/2	5/8	5	12	5	2 1/2	2 1/2	4	12	9 1/4														7.5
3	36	48	1 3/8	1/2	5/8	6	14	6	5	5	6	14 3/8	11														12.0
4	36	48	1 3/8	1/2	5/8	6	14	6	5	5	6	14 3/8	11														12.0
5	48	60	2 1/4	3/4	1	8	20	6	4 1/2	6 3/4	6 3/4	19 1/4	14 5/8														20.0

STANDARD SIGN
R2-1

WISCONSIN DEPT OF TRANSPORTATION

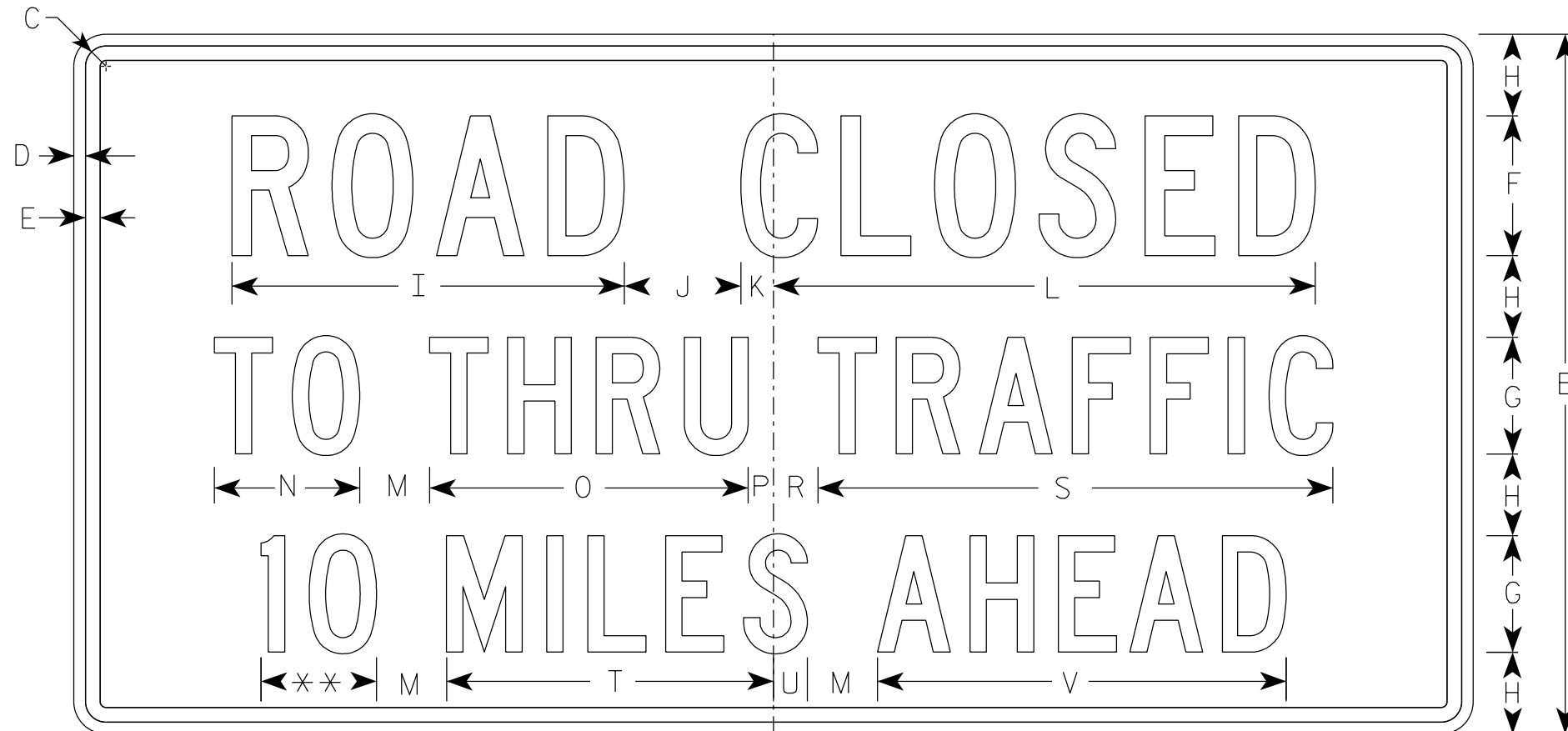
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 5/26/10 PLATE NO. R2-1.13

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E

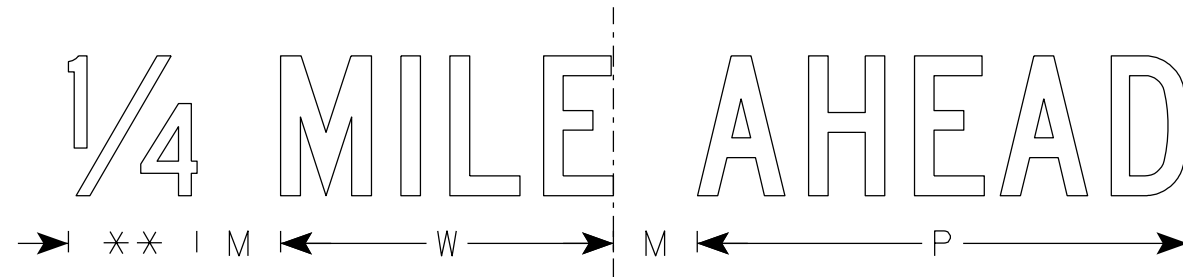
NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals to nearest quarter mile and optically adjust spacing to achieve proper balance.



R11-3

** See Note 5



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.	
1	36	18	1 1/4	3/8	3/8	4	3	2	11 1/4	3	1 1/8	15 3/8	2	3 3/4	8 1/4	5/8		1 3/8	13 1/4	8 3/8	7/8	10 1/2	7 1/8				4.5	
2S	60	30	1 3/8	1/2	5/8	6	5	3 1/2	16 7/8	5	1 3/8	23 1/4	3	6 1/4	13 5/8	1 1/8		1 7/8	22 1/8	14	1 1/2	17 1/2	11 7/8				12.5	
2M	60	30	1 3/8	1/2	5/8	6	5	3 1/2	16 7/8	5	1 3/8	23 1/4	3	6 1/4	13 5/8	1 1/8		1 7/8	22 1/8	14	1 1/2	17 1/2	11 7/8				12.5	
3																												
4																												
5																												

STANDARD SIGN
R11-3

WISCONSIN DEPT OF TRANSPORTATION

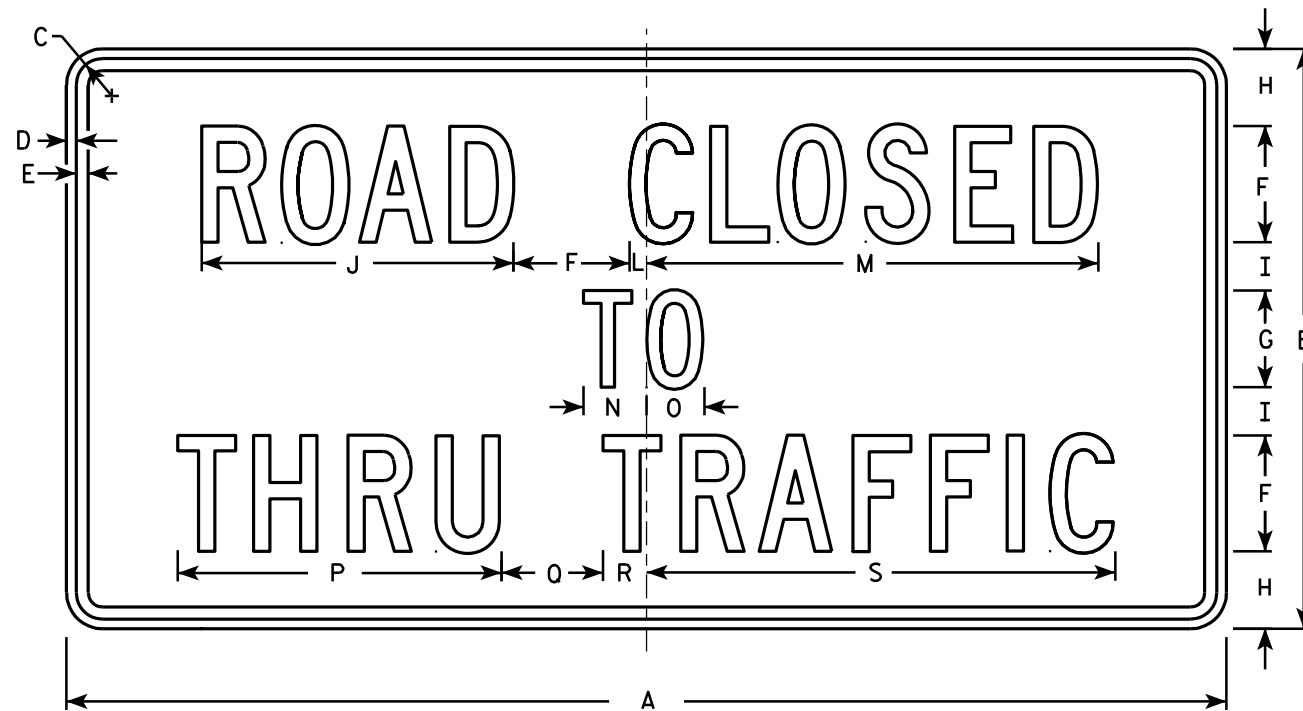
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 6/14/2021 PLATE NO. R11-3.9

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ **E**

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



R11-4

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	60	30	1 3/8	1/2	5/8	6	5	4	2 1/2	16 1/8		7/8	23 3/8	3 1/4	3	16 3/4	5 1/4	2 1/4	24 1/4								12.5
2M	60	30	1 3/8	1/2	5/8	6	5	4	2 1/2	16 1/8		7/8	23 3/8	3 1/4	3	16 3/4	5 1/4	2 1/4	24 1/4								12.5
3																											
4																											
5																											

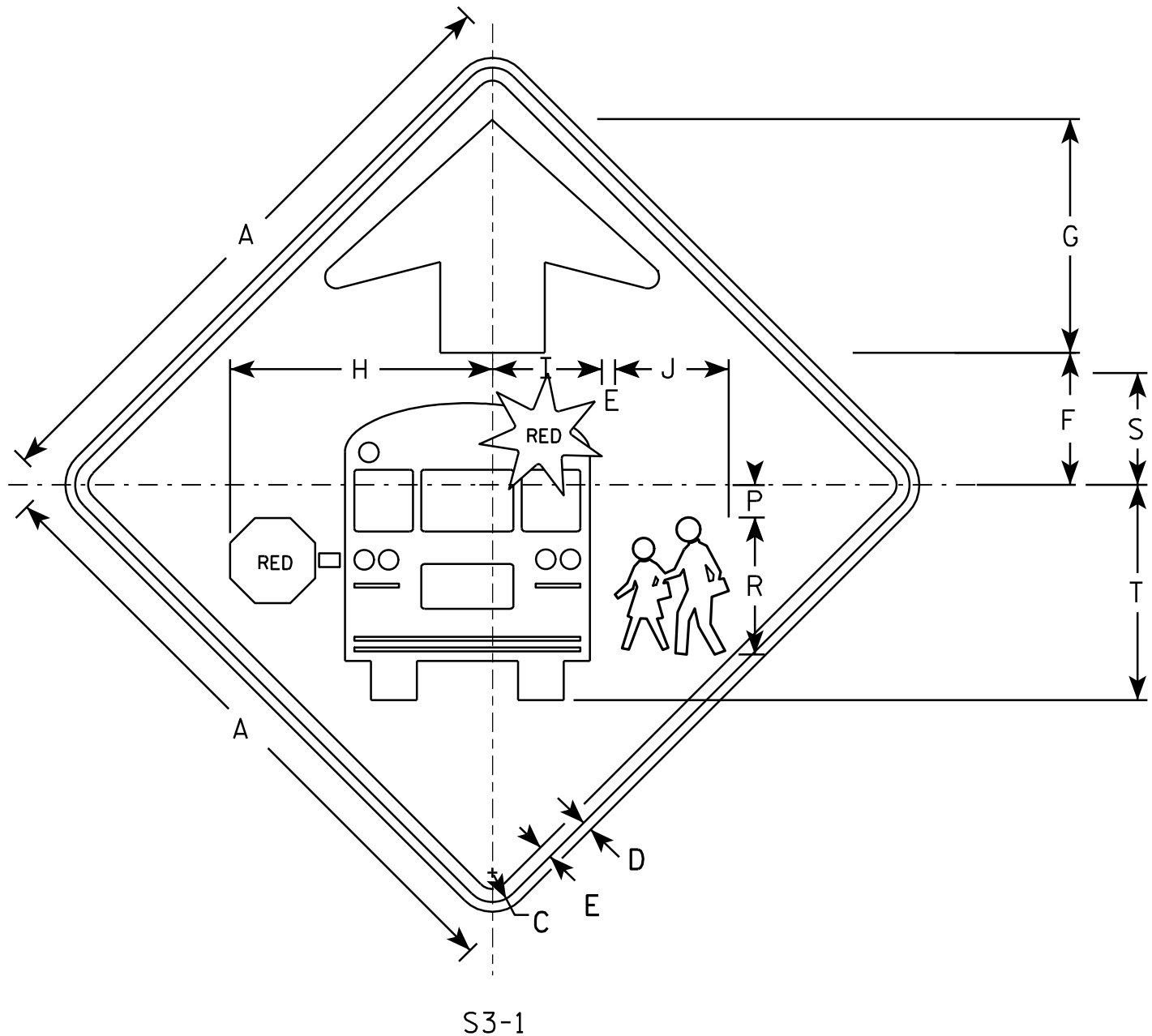
STANDARD SIGN
R11 - 4

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Raush*
for State Traffic Engineer

DATE 4/1/11 PLATE NO. R11-4.3

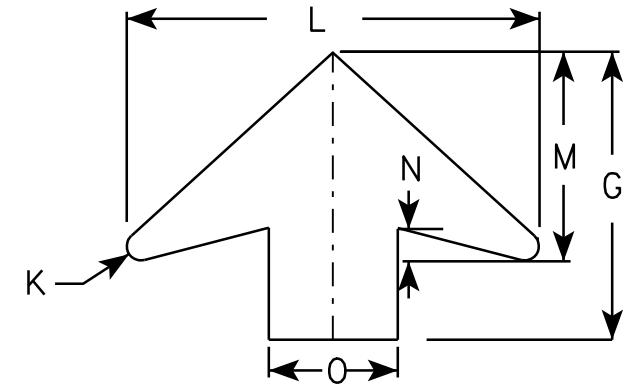
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E



S3-1

NOTES

1. All Signs Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
 Background - YELLOW-GREEN
 Message - BLACK except as noted
 Circles except PEDS- RED BACKGROUND
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



ARROW DETAIL

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30		1 3/8	1/2	5/8	6 1/4	11 1/4	12 1/2	5 1/4	5 1/2	1/2	16	8	1 1/4	5	1 1/2		6 5/8	5 3/8	10 3/8							6.25
2	36		1 5/8	5/8	3/4	7 1/2	13 1/2	15 1/8	6 1/4	6 1/2	5/8	19 1/4	9 3/4	1 5/8	6	1 7/8		7 1/8	6 3/8	12 3/8							9.0
3	48		2 1/4	3/4	1	10	17 7/8	20 1/8	8 3/8	8 3/4	7/8	25 5/8	13	2	8	2 1/2		10 1/2	8 1/2	16 1/2							16.0
4	48		2 1/4	3/4	1	10	17 7/8	20 1/8	8 3/8	8 3/4	7/8	25 5/8	13	2	8	2 1/2		10 1/2	8 1/2	16 1/2							16.0
5																											

STANDARD SIGN
S3-1

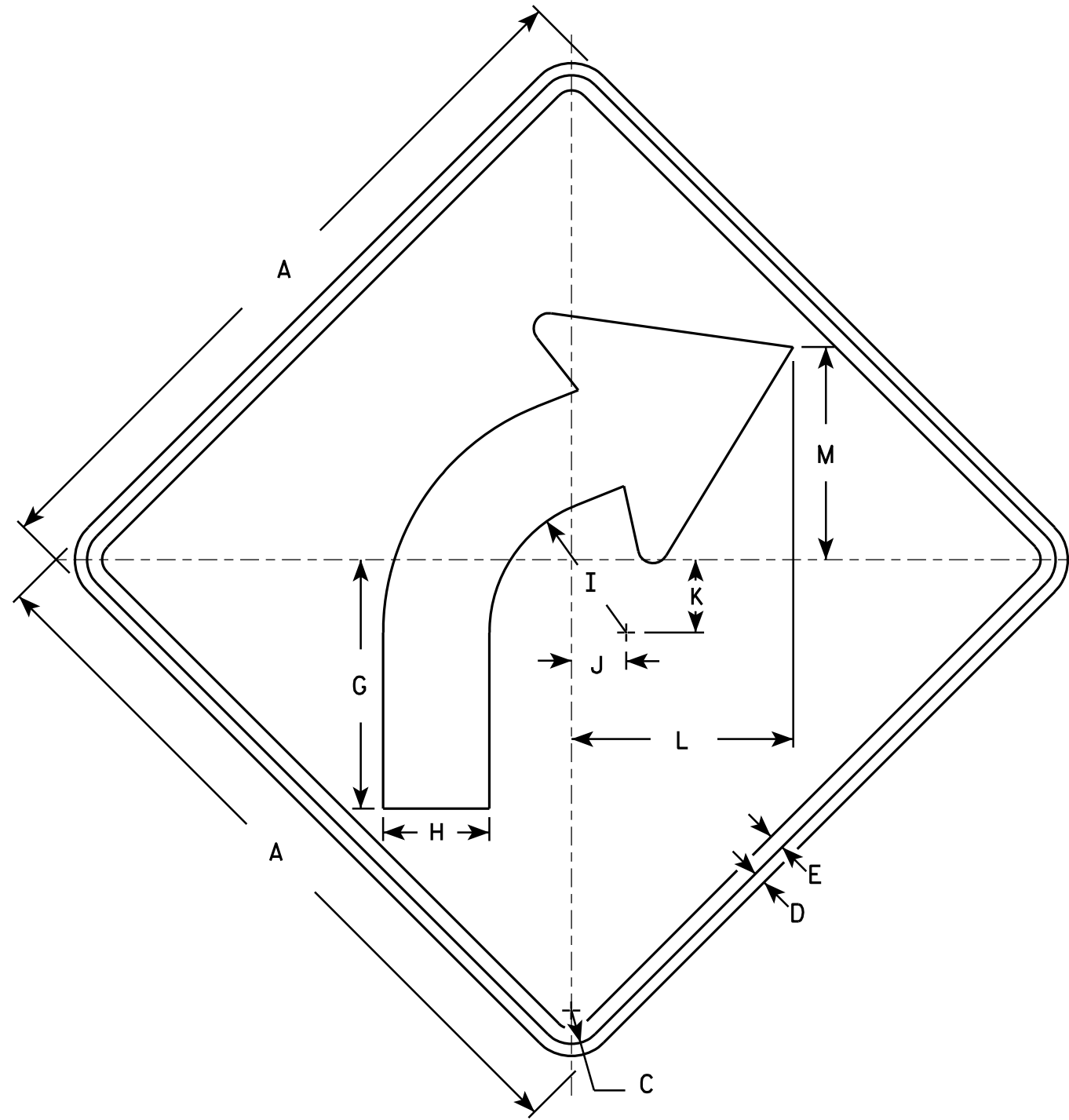
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

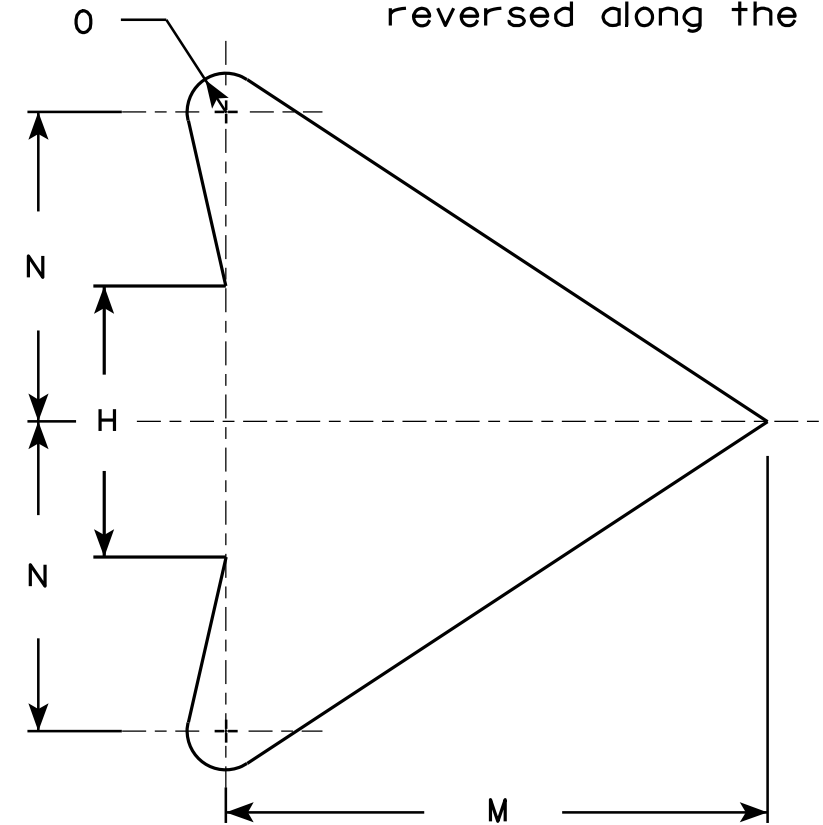
DATE 6/8/10 PLATE NO. S3-1.6

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. W1-2L is the same as W1-2R except the arrow is reversed along the vertical centerline.



W1-2R



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2		8 1/4	3 1/2	4 1/2	1 3/4	2 3/8	7 1/4	7	4	1/2												4.0
2S	30		1 3/8	1/2	5/8		10 1/4	4 3/8	5 5/8	2 1/4	3	9 1/8	8 3/4	5	5/8												6.25
2M	36		1 5/8	5/8	3/4		12 3/8	5 1/4	6 3/4	2 5/8	3 1/2	10 7/8	10 1/2	6	3/4												9.0
3	36		1 5/8	5/8	3/4		12 3/8	5 1/4	6 3/4	2 5/8	3 1/2	10 7/8	10 1/2	6	3/4												9.0
4	36		1 5/8	5/8	3/4		12 3/8	5 1/4	6 3/4	2 5/8	3 1/2	10 7/8	10 1/2	6	3/4												9.0
5	48		2 1/4	3/4	1		16 1/2	7	9	3 1/2	4 5/8	14 1/2	14	8	1												16.0

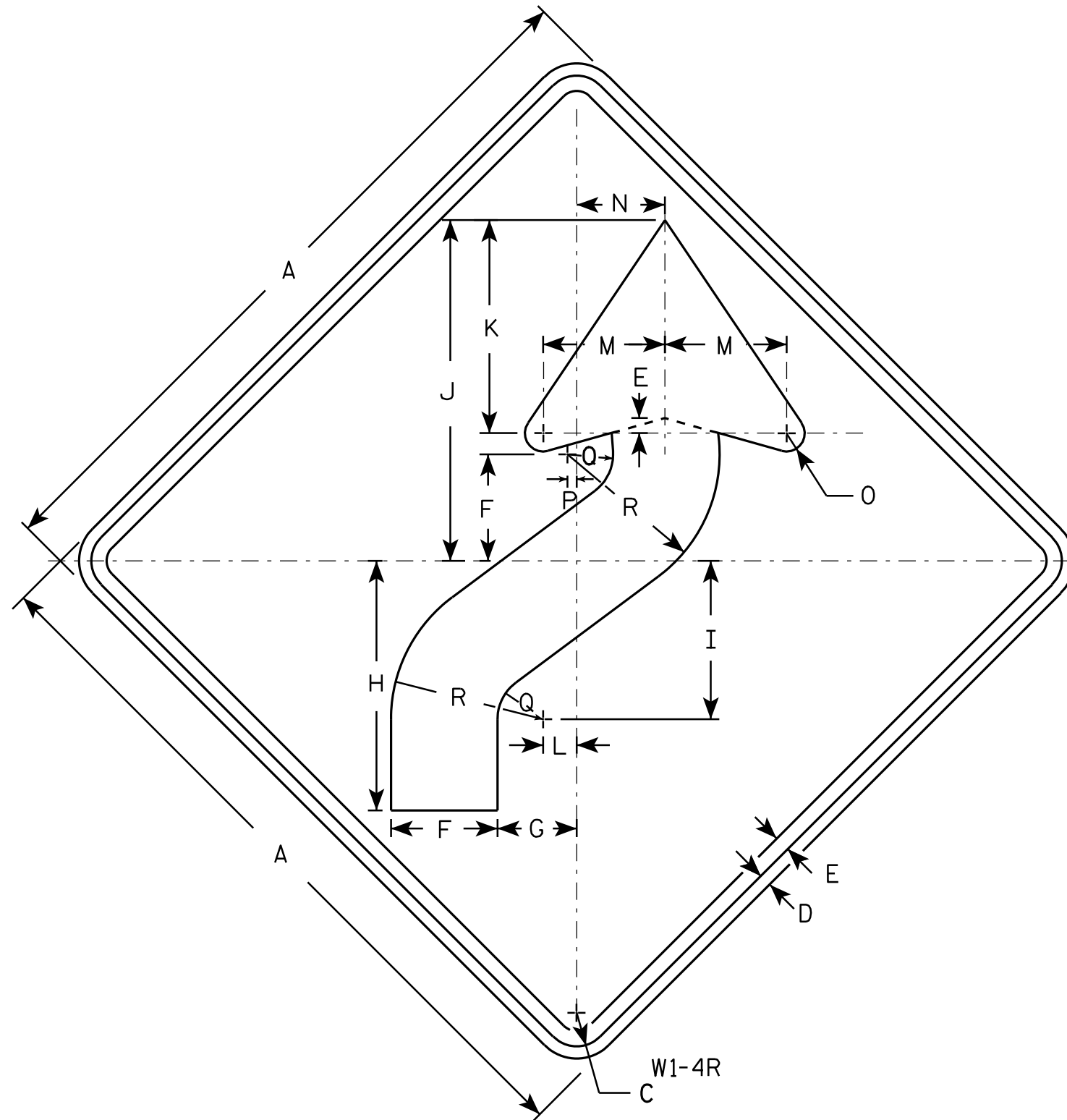
STANDARD SIGN
W1-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 5/15/12 PLATE NO. W1-2.10

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. W1-4L is the same as W1-4R except the arrow is reversed along the vertical centerline.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2	3 1/2	2 5/8	8 1/4	5 1/4	11 1/4	7	1 1/8	4	3	5/8	1/4	1 1/2	5									4.0
2S	30		1 3/8	1/2	5/8	4 3/8	3 1/4	10 1/4	6 1/2	14	8 3/4	1 3/8	5	3 5/8	3/4	3/8	1 7/8	6 1/4									6.25
2M	36		1 5/8	5/8	3/4	5 1/4	4	12 3/8	7 7/8	16 7/8	10 1/2	1 5/8	6	4 1/2	1	1/2	2 1/4	7 1/2									9.0
3	36		1 5/8	5/8	3/4	5 1/4	4	12 3/8	7 7/8	16 7/8	10 1/2	1 5/8	6	4 1/2	1	1/2	2 1/4	7 1/2									9.0
4	36		1 5/8	5/8	3/4	5 1/4	4	12 3/8	7 7/8	16 7/8	10 1/2	1 5/8	6	4 1/2	1	1/2	2 1/4	7 1/2									9.0
5	48		2 1/4	3/4	1	7	5 1/4	16 1/2	10 1/2	22 1/2	14	2 1/4	8	6	1 1/4	5/8	3	10									16.0

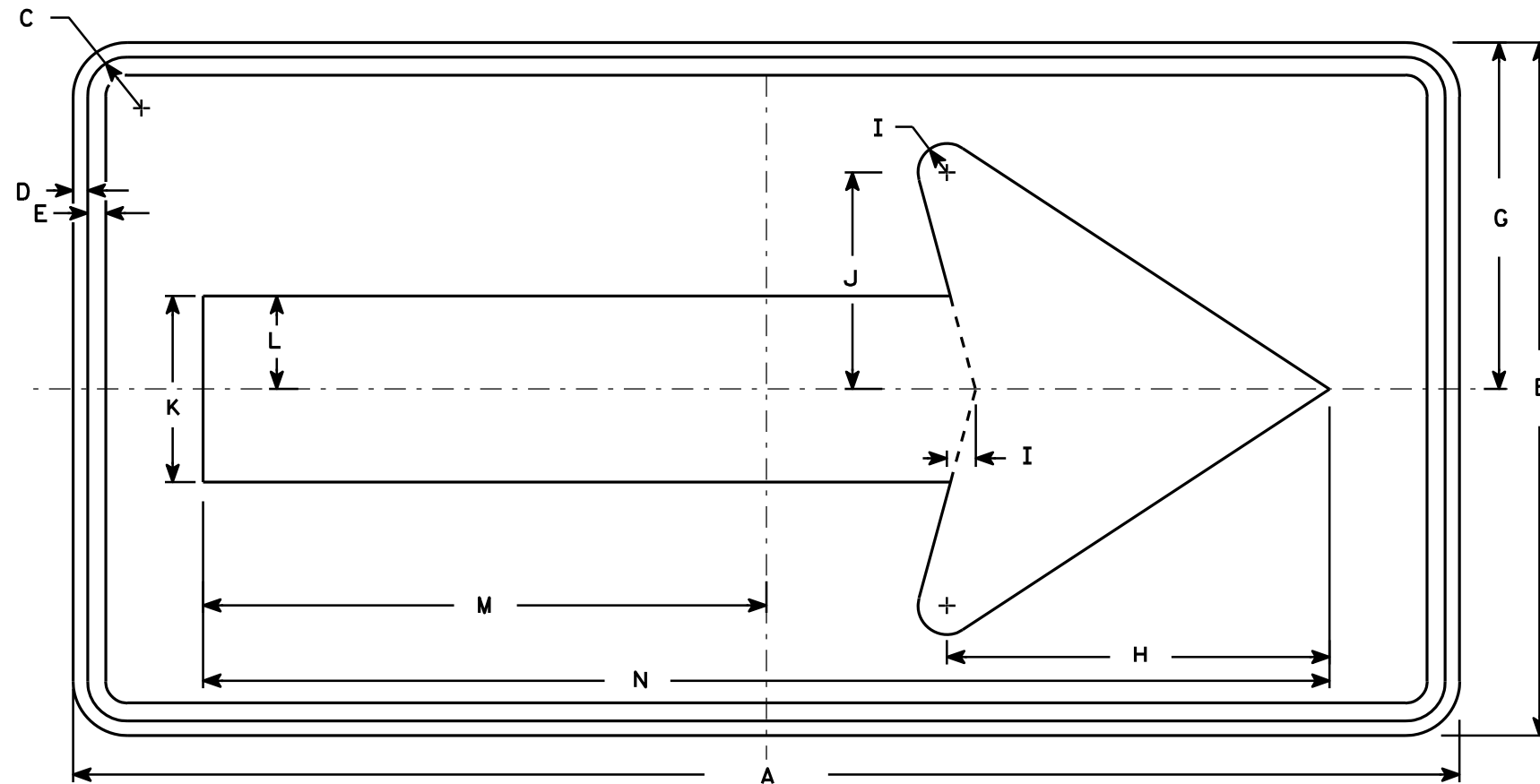
STANDARD SIGN
W1 - 4

WISCONSIN DEPT OF TRANSPORTATION
APPROVED *Matthew R. Raub*
for State Traffic Engineer
DATE 5/17/12 PLATE NO. W1-4.11

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



W1-6

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36	18	1 1/8	3/8	3/8		9	10	3/4	5 5/8	4 3/4	2 3/8	14 5/8	29 1/4													4.5
2S	48	24	1 3/8	1/2	5/8		12	13 1/4	1	7 1/2	6 1/2	3 1/4	19 1/2	39													8.0
2M	48	24	1 3/8	1/2	5/8		12	13 1/4	1	7 1/2	6 1/2	3 1/4	19 1/2	39													8.0
3	60	30	1 3/8	1/2	5/8		15	16 1/4	1 1/4	9 1/4	8	4	24 3/8	48 3/4													12.5
4	60	30	1 3/8	1/2	5/8		15	16 1/4	1 1/4	9 1/4	8	4	24 3/8	48 3/4													12.5
5	96	48	2 1/4	3/4	1		24	26 1/2	2	15	13	6 1/2	39	78													32.0

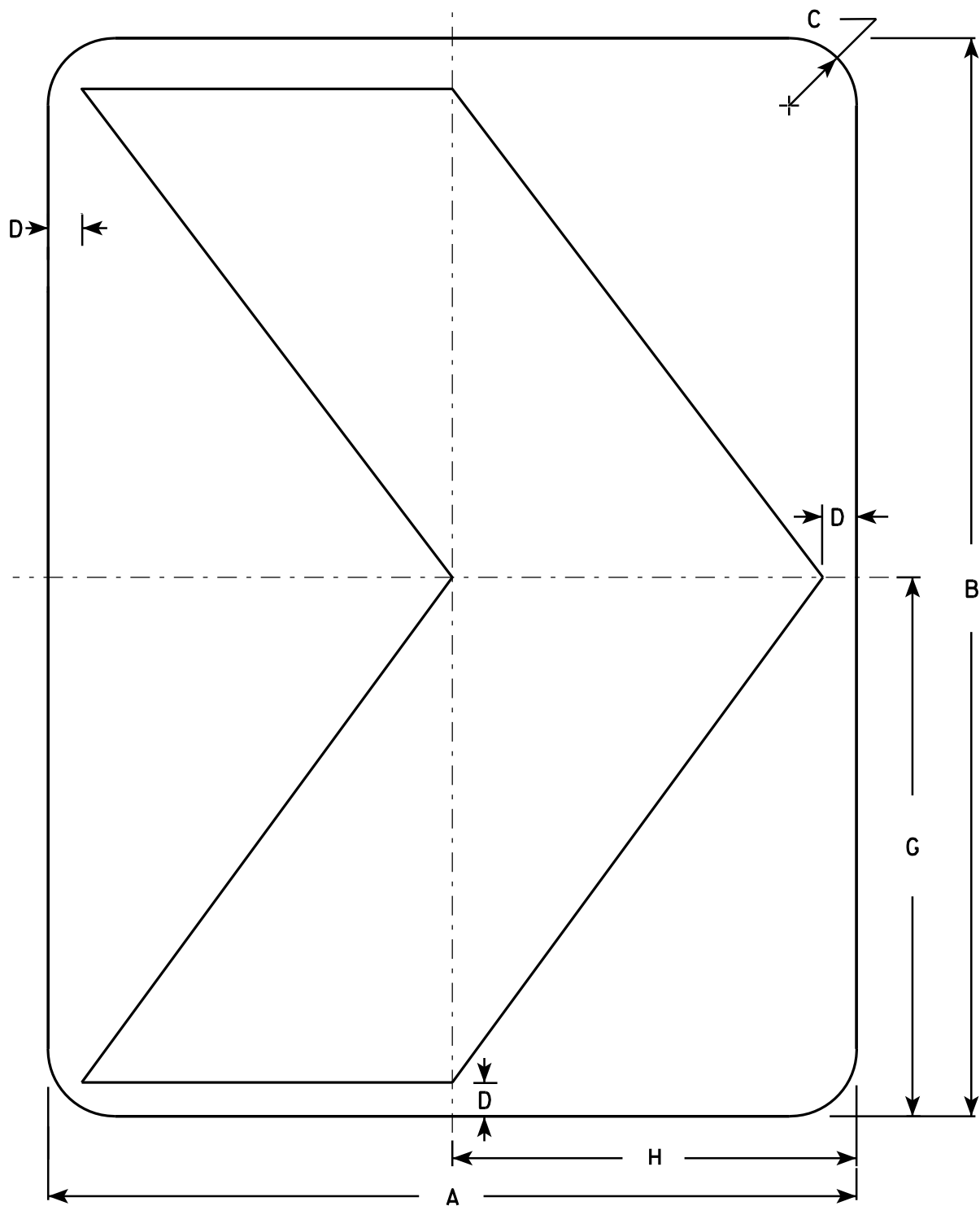
STANDARD SIGN
W1-6

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 6/7/10 PLATE NO. W1-6.8

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

7

7

W1-8

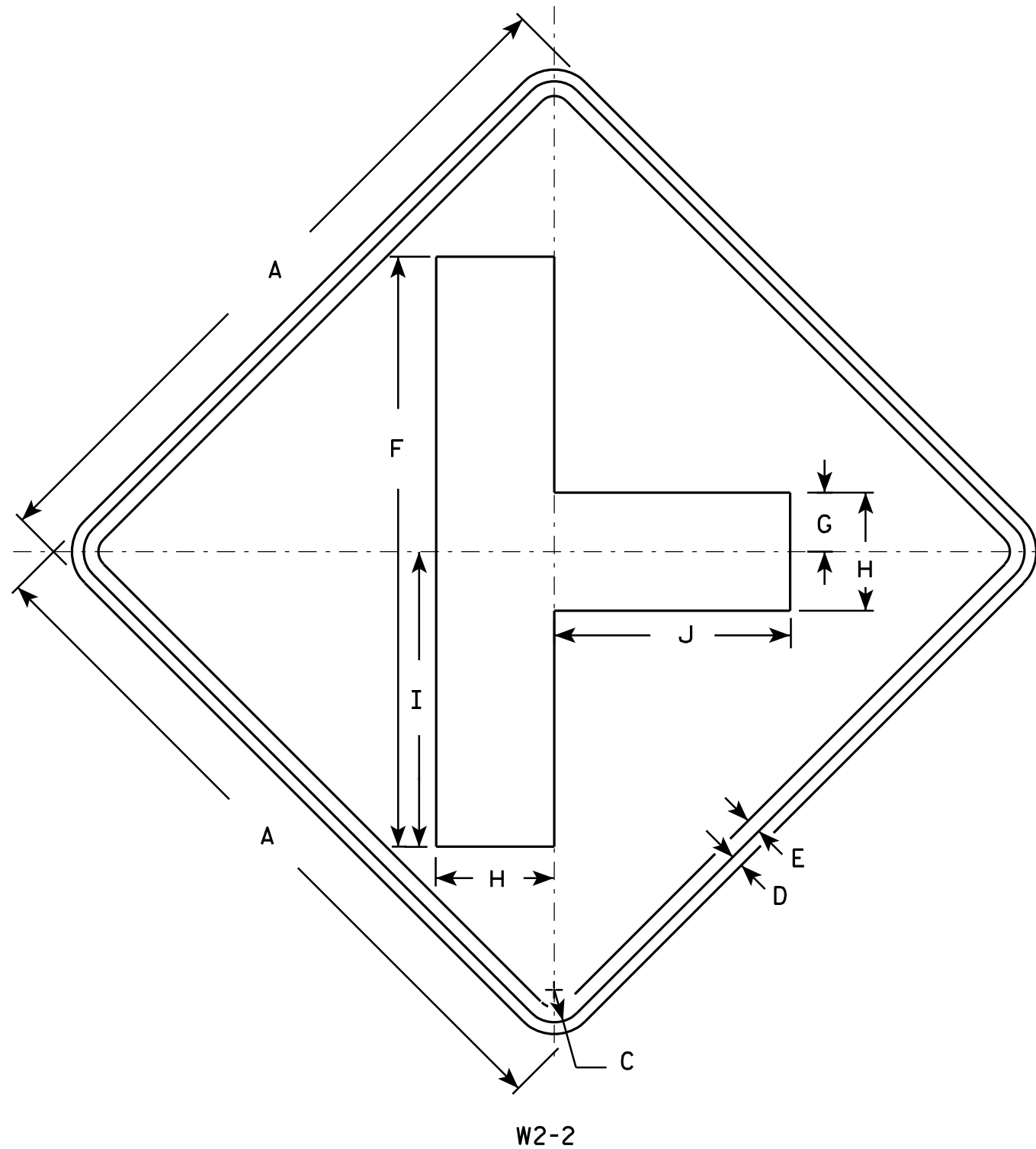
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	12	18	1 1/2	1/2			9	6																			1.5
2S	18	24	1 1/2	3/4			12	9																			3.0
2M	18	24	1 1/2	3/4			12	9																			3.0
3	24	30	1 1/2	1			15	12																			5.0
4	30	36	1 7/8	1 1/4			18	15																			7.5
5	36	48	2 1/4	1 1/2			24	18																			12.0

STANDARD SIGN
W1-8

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 6/7/10 PLATE NO. W1-8.6



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

7

7

W2-2

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2	20	2	4	10	8																	4.0
2S	30		1 3/8	1/2	5/8	25	2 1/2	5	12 1/2	10																	6.25
2M	30		1 3/8	1/2	5/8	25	2 1/2	5	12 1/2	10																	6.25
3	36		1 5/8	5/8	3/4	30	3	6	15	12																	9.0
4	48		2 1/4	3/4	1	40	4	8	20	16																	16.0
5																											

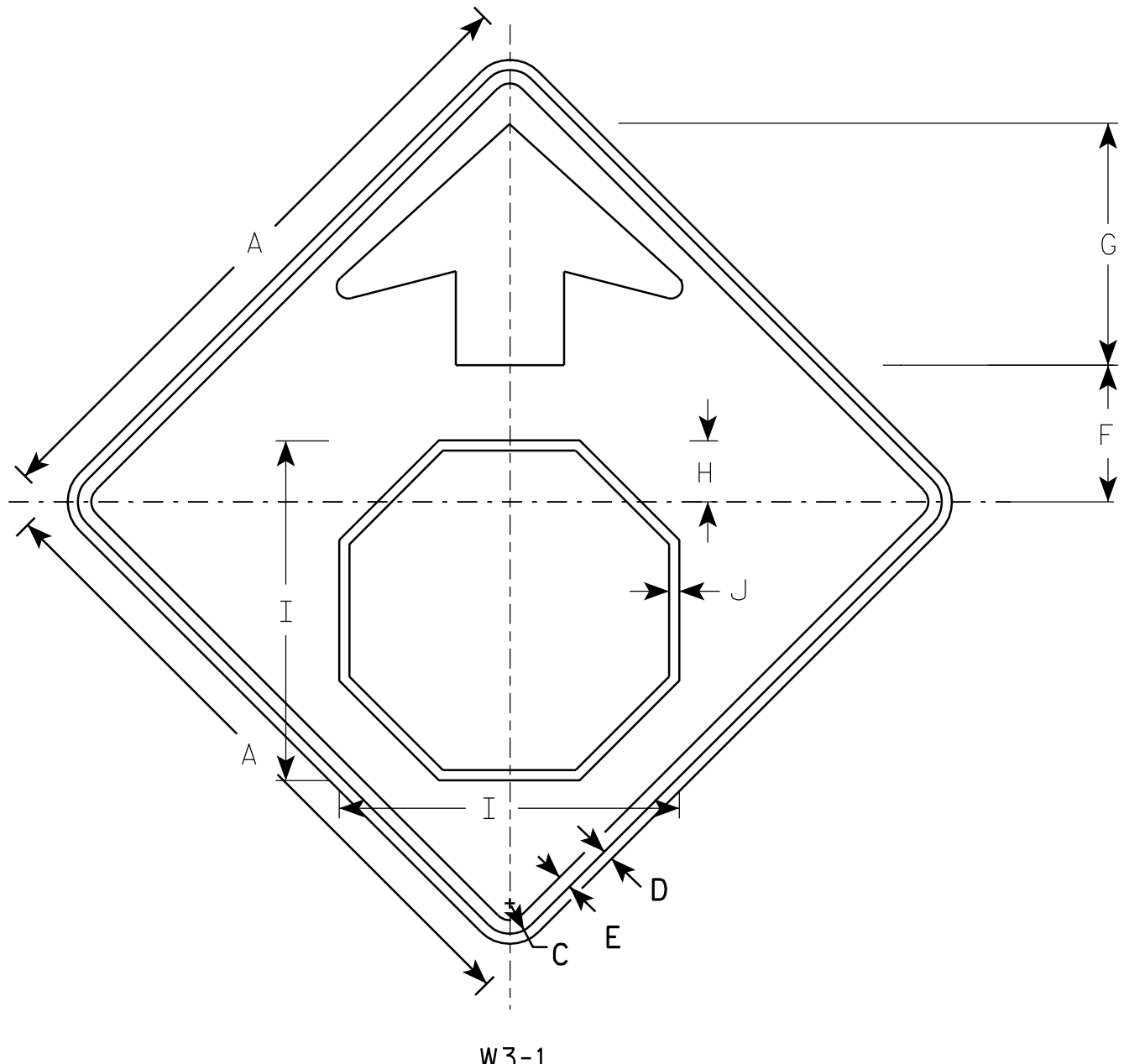
STANDARD SIGN
W2-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 5/29/12 PLATE NO. W2-2.6

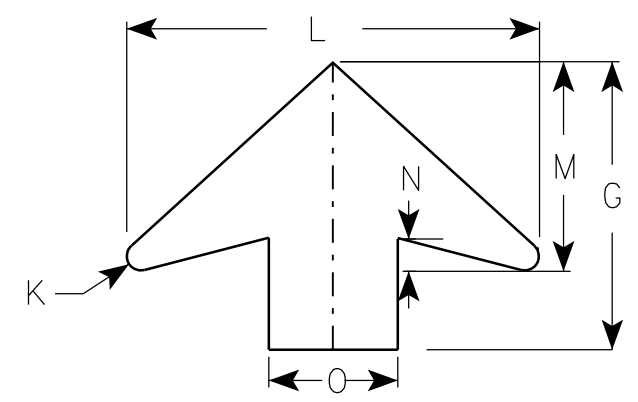
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ **E**



W3-1

NOTES

1. All Signs Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
 Background - YELLOW
 Arrow & Border - BLACK
 Stop Symbol - WHITE BORDER ON RED BACKGROUND



ARROW DETAIL

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30		1 3/8	1/2	5/8	6 1/4	11 1/4	2 7/8	15 3/4	1/2	1/2	16	8	1 1/4	5												6.25
2S	36		1 5/8	5/8	3/4	7 1/2	13 1/2	3 1/2	19	5/8	5/8	19 1/4	9 3/4	1 5/8	6												9.0
2M	36		1 5/8	5/8	3/4	7 1/2	13 1/2	3 1/2	19	5/8	5/8	19 1/4	9 3/4	1 5/8	6												9.0
3	36		1 5/8	5/8	3/4	7 1/2	13 1/2	3 1/2	19	5/8	5/8	19 1/4	9 3/4	1 5/8	6												9.0
4	48		2 1/4	3/4	1	10	17 7/8	4 1/2	25 1/8	3/4	7/8	25 5/8	13	2	8												16.0
5	48		2 1/4	3/4	1	10	17 7/8	4 1/2	25 1/8	3/4	7/8	25 5/8	13	2	8												16.0

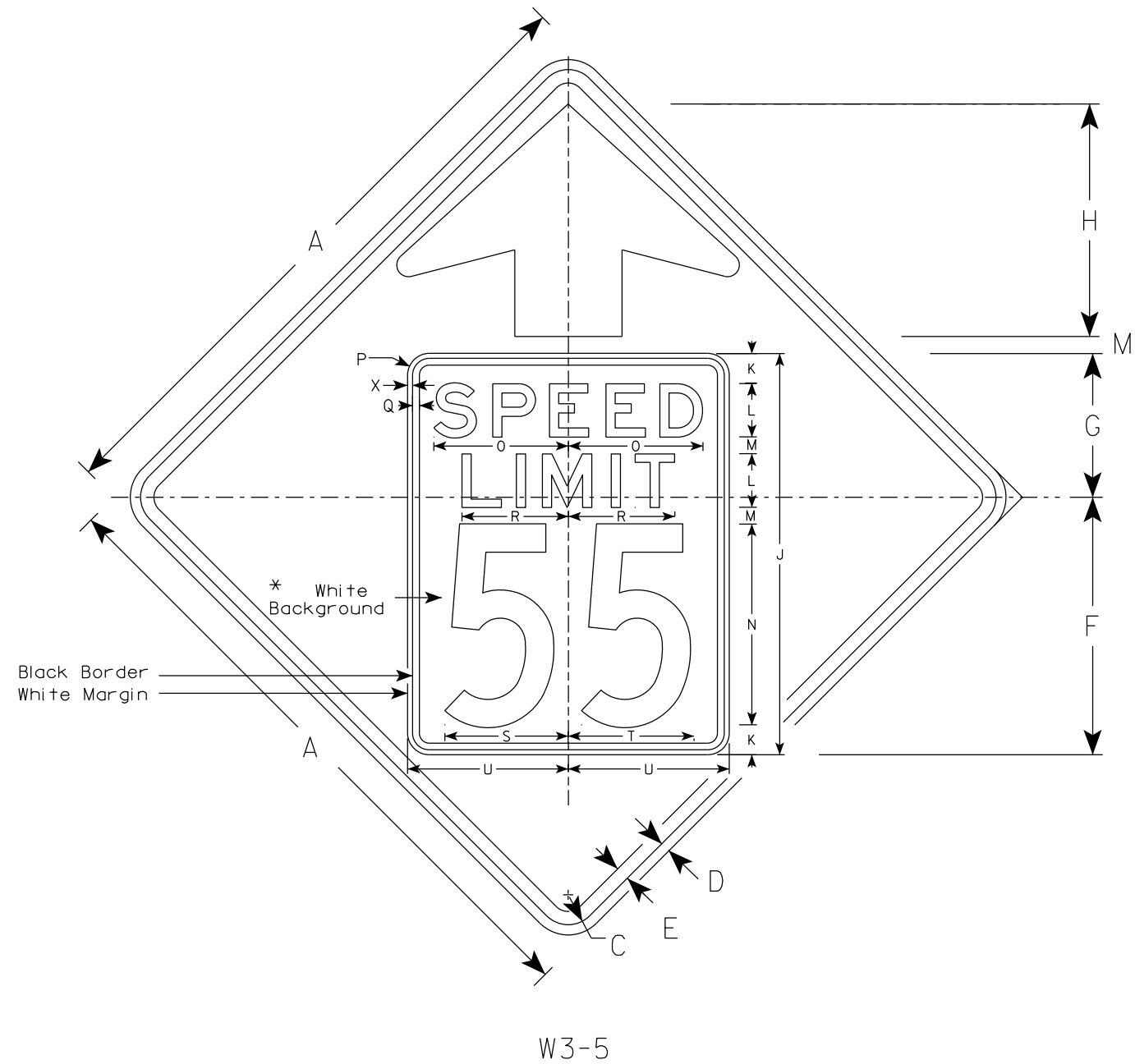
STANDARD SIGN
W3-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 6/7/10 PLATE NO. W3-1.12

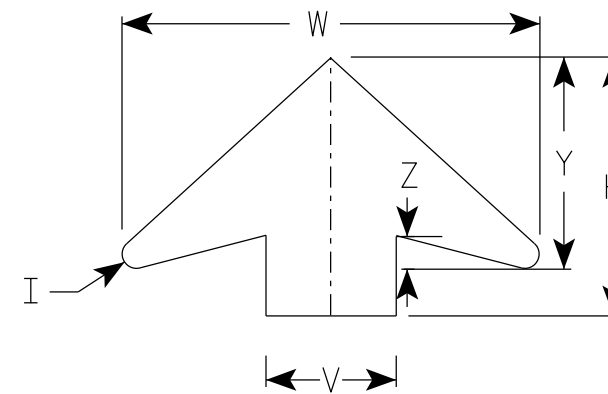
PROJECT NO: _____ SHEET NO: _____ E



NOTES

1. Sign is Type II - See Note 2 for Sheeting Type
2. Color: *
Background - Yellow* (Type F Reflective)
Message - Black
3. Message Series - C for numbers Series E for wording
4. Substitute appropriate numerals and optically adjust spacing to achieve proper balance

*Speed Limit Sign shall have a White Background with black message (Type SH Reflective)



ARROW DETAIL

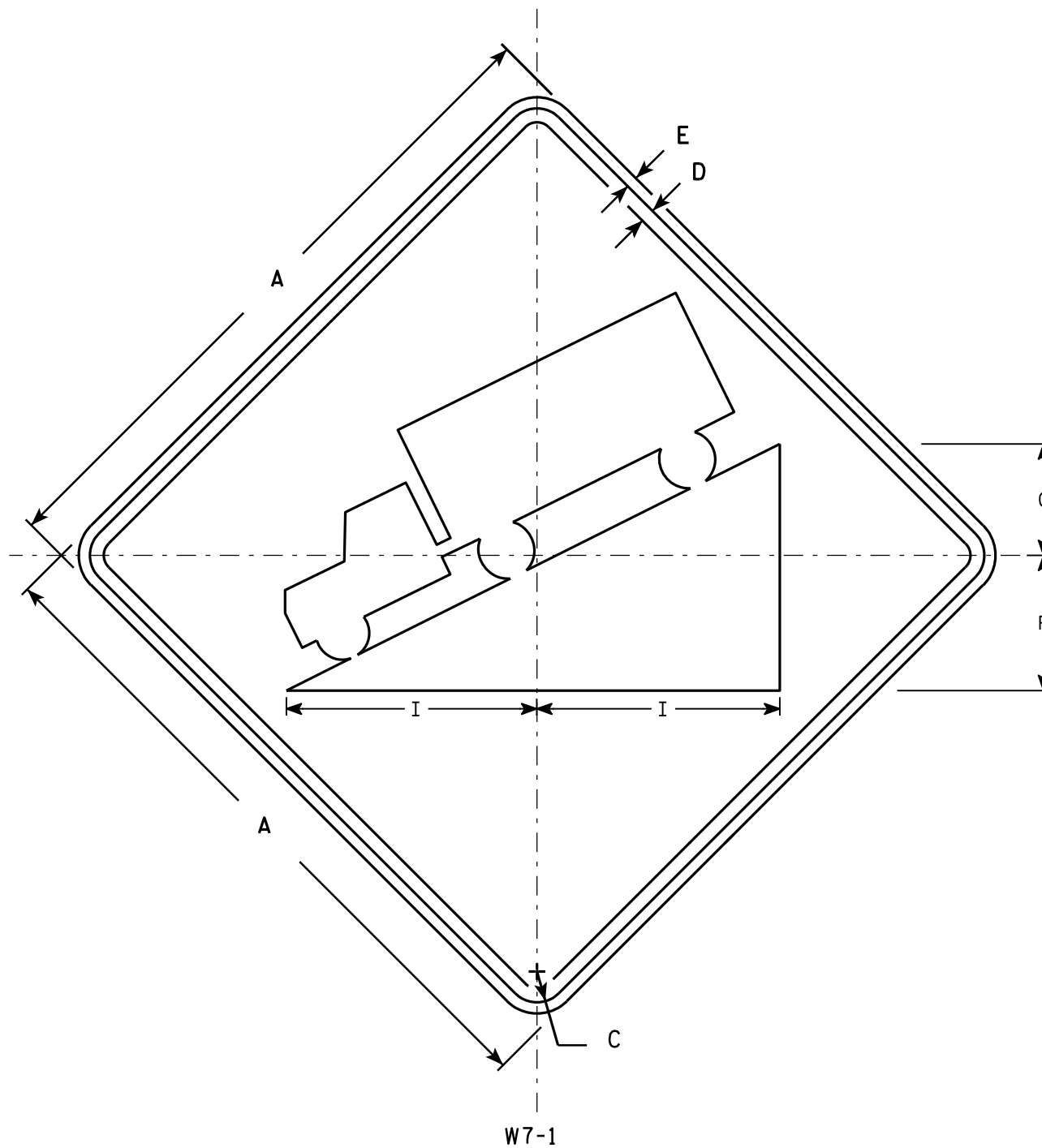
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	36		1 5/8	5/8	3/4	14 1/2	9 1/2	11 1/2	5/8	24	2	3	1	12	7 1/8	1 1/2	3/8	5 3/4	7 1/4	7 1/8	9	6	19 1/4	3/8	9 3/4	1 5/8	9.0
2M	36		1 5/8	5/8	3/4	14 1/2	9 1/2	11 1/2	5/8	24	2	3	1	12	7 1/8	1 1/2	3/8	5 3/4	7 1/4	7 1/8	9	6	19 1/4	3/8	9 3/4	1 5/8	9.0
3	36		1 5/8	5/8	3/4	14 1/2	9 1/2	11 1/2	5/8	24	2	3	1	12	7 1/8	1 1/2	3/8	5 3/4	7 1/4	7 1/8	9	6	19 1/4	3/8	9 3/4	1 5/8	9.0
4	48		2 1/4	3/4	1	19 1/4	10 3/4	17 3/8	7/8	30	2 1/4	4	1 1/4	15	10	1 5/8	1/2	8	9 1/4	9 3/8	12	8	25 5/8	3/8	13	2	16.0
5	48		2 1/4	3/4	1	19 1/4	10 3/4	17 3/8	7/8	30	2 1/4	4	1 1/4	15	10	1 5/8	1/2	8	9 1/4	9 3/8	12	8	25 5/8	3/8	13	2	16.0

STANDARD SIGN
W3-5

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 7/27/2020 PLATE NO. W3-5.6



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	3/8	5	4		8 3/4																		4.0
2S	30		1 3/8	1/2	5/8	6	5		11																		6.25
2M	36		1 5/8	5/8	3/4	7 1/4	6		13 1/4																		9.0
3	36		1 5/8	5/8	3/4	7 1/4	6		13 1/4																		9.0
4	36		1 5/8	5/8	3/4	7 1/4	6		13 1/4																		9.0
5	48		2 1/4	3/4	1	9 3/4	8		17 1/2																		16.0

STANDARD SIGN
W7-1

WISCONSIN DEPT OF TRANSPORTATION

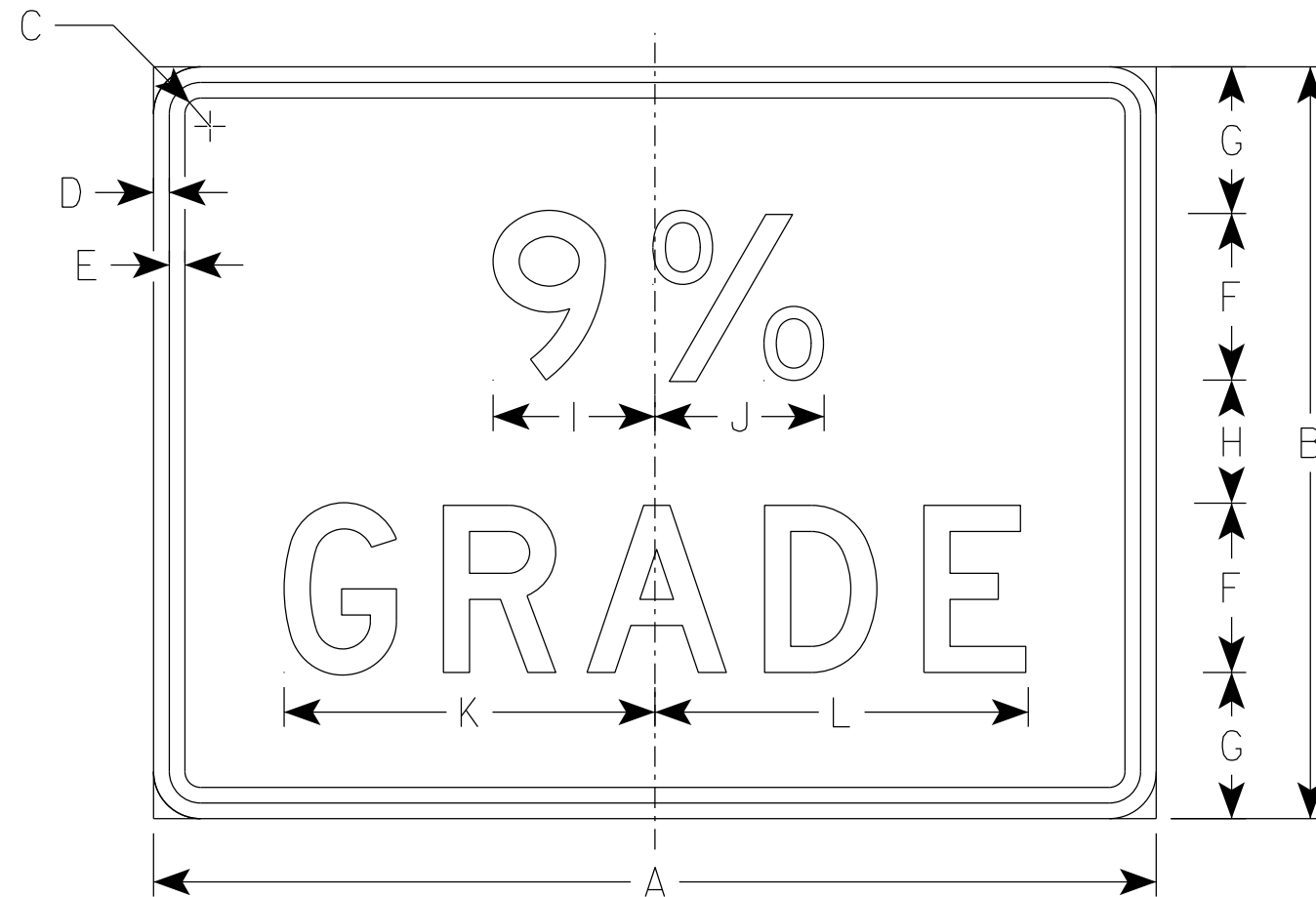
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 03/12/13 PLATE NO. W7-1.13

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Yellow
Message - Black
3. Message Series - D
4. Substitute appropriate numeral and optically adjust spacing to achieve proper balance.



W7-3

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	18	1 1/8	3/8	3/8	4	3 1/2	3	3 7/8	4	8 7/8	9															3.0
2M	24	18	1 1/8	3/8	3/8	4	3 1/2	3	3 7/8	4	8 7/8	9															3.0
3																											
4																											
5																											

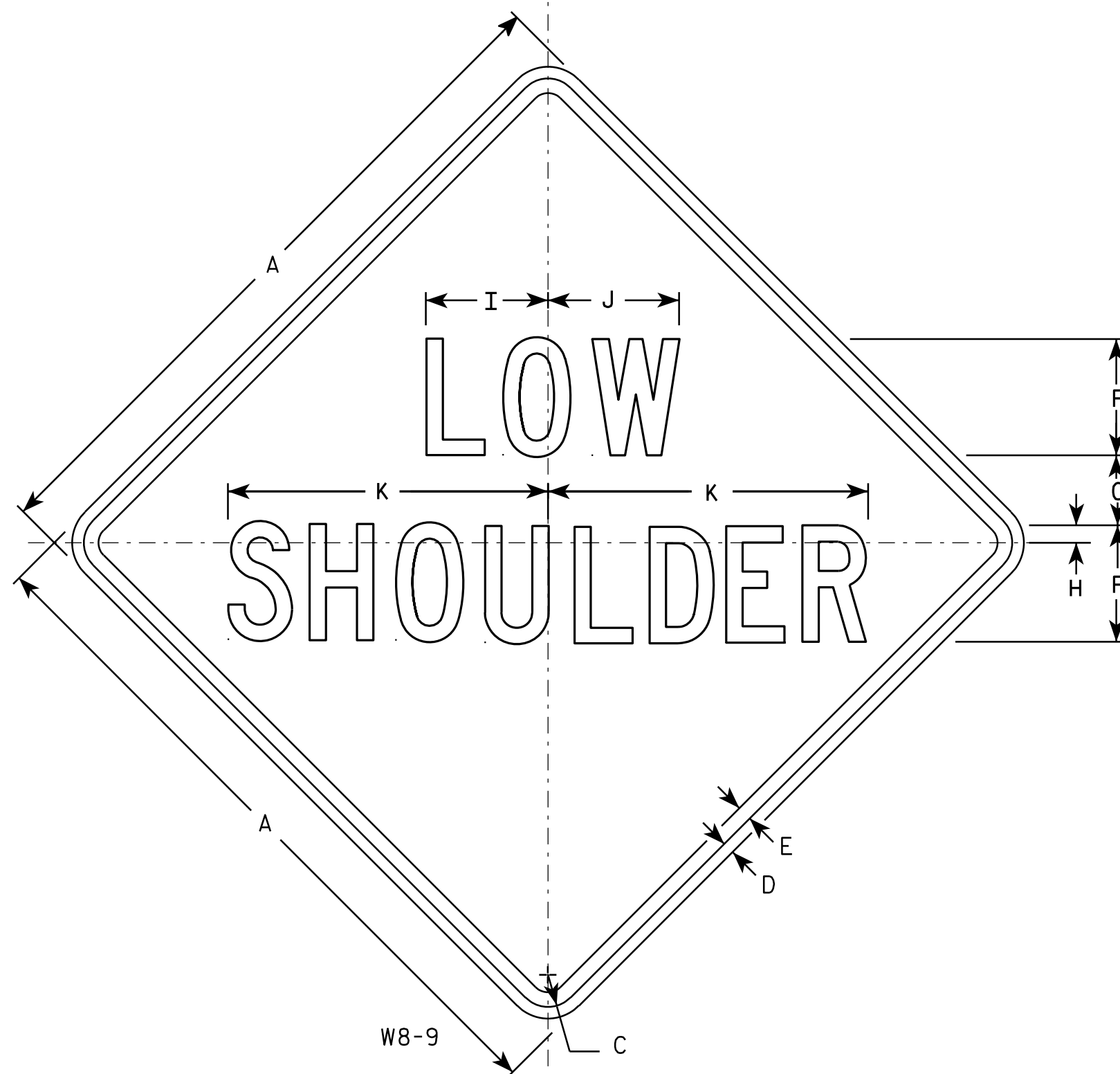
STANDARD SIGN
W7-3

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 8/23/18 PLATE NO. W7-3.5

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ **E**



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2	4	2 1/2	1/2	4 1/8	4 1/2	11																4.0
2S	36		1 5/8	5/8	3/4	6	3 1/2	1	6 1/4	6 3/4	16 1/2																9.0
2M	36		1 5/8	5/8	3/4	6	3 1/2	1	6 1/4	6 3/4	16 1/2																9.0
3	36		1 5/8	5/8	3/4	6	3 1/2	1	6 1/4	6 3/4	16 1/2																9.0
4	36		1 5/8	5/8	3/4	6	3 1/2	1	6 1/4	6 3/4	16 1/2																9.0
5	48		2 1/4	3/4	1	8	5	1 1/4	8 1/4	9	21 7/8																16.0

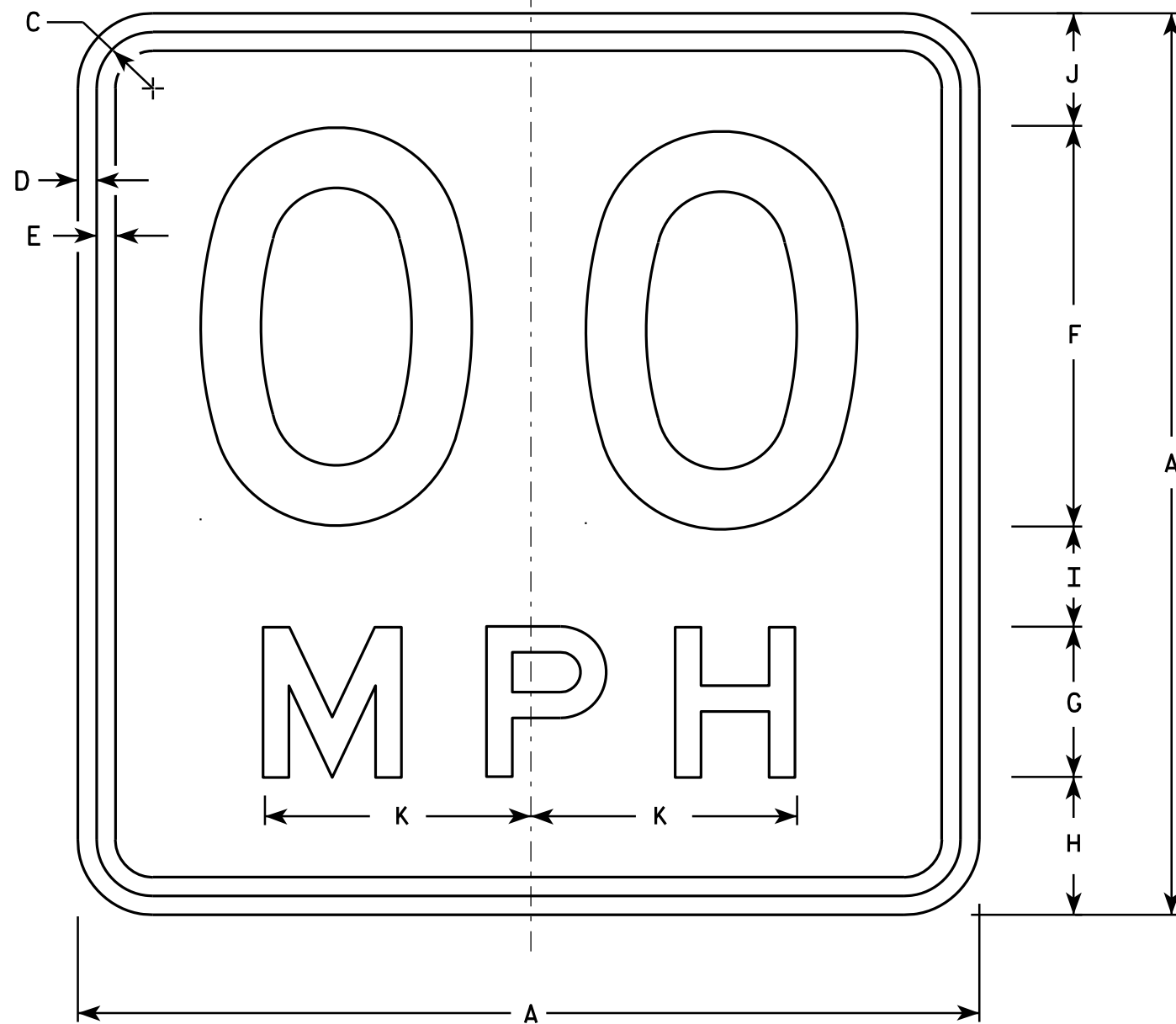
STANDARD SIGN
W8-9

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 03/14/13 PLATE NO. W8-9.9

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Message Series - See Note 6
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals and optically space about centerline to achieve proper balance.
6. Line 1 is Series D
Line 2 is Series E

W13-1

* For 30" x 30" Warning Signs, use 18" x 18" W13-1 signs.
For 36" x 36" Warning Signs, use 24" x 24" W13-1 signs.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18		1 1/8	3/8	3/8	8	3	2 3/4	2	2 1/4	5 3/8															2.25	
* 2S	18		1 1/8	3/8	3/8	8	3	2 3/4	2	2 1/4	5 3/8															2.25	
* 2M	18		1 1/8	3/8	3/8	8	3	2 3/4	2	2 1/4	5 3/8															2.25	
3	24		1 1/8	3/8	1/2	10	4	4	2 3/4	3 1/4	6 5/8															4.00	
4	36		1 5/8	5/8	3/4	16	6	5 1/2	4	4 1/2	10 5/8															9.00	
5	36		1 5/8	5/8	3/4	16	6	5 1/2	4	4 1/2	10 5/8															9.00	

STANDARD SIGN

W13-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 5/31/12 PLATE NO. W13-1.16

PROJECT NO:

HWY:

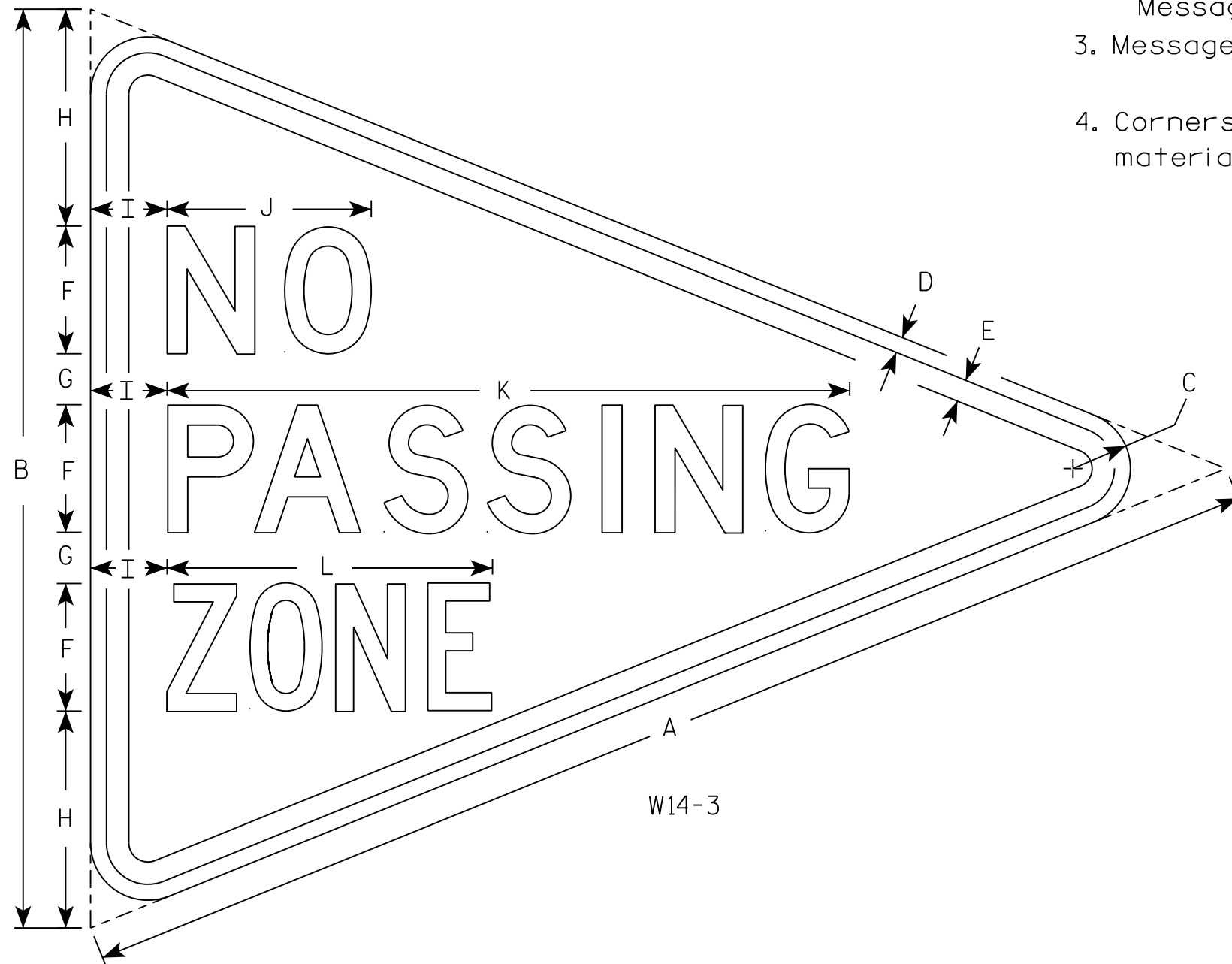
COUNTY:

SHEET NO:

E

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Yellow
Message - Black
3. Message Series - Lines 1 and 2 are Series D.
Line 3 is series C.
4. Corners and borders shall be rounded on all base materials for this sign.



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48	36	2 1/4	5/8	7/8	5	2	8 1/2	3	8	26 3/4	12 3/4															5.56
2M																											
3																											
4																											
5																											

STANDARD SIGN
W14-3

WISCONSIN DEPT OF TRANSPORTATION

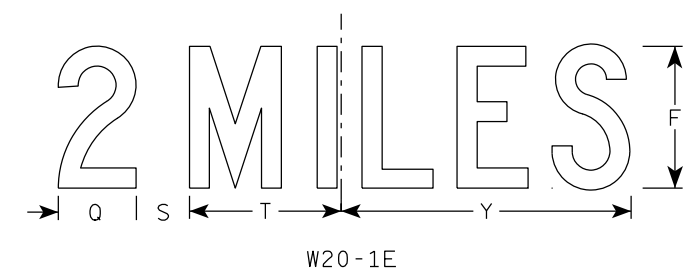
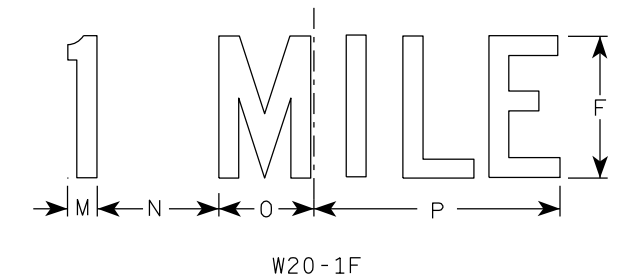
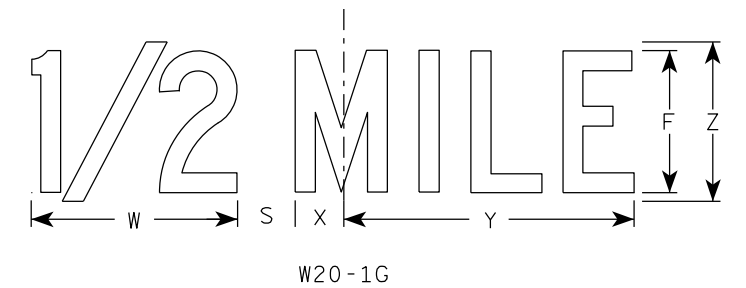
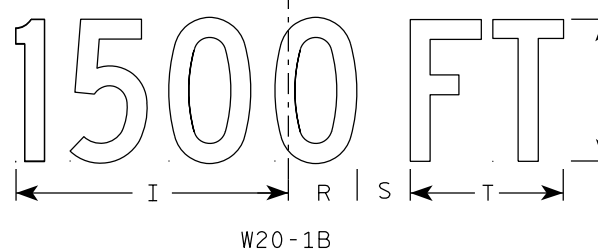
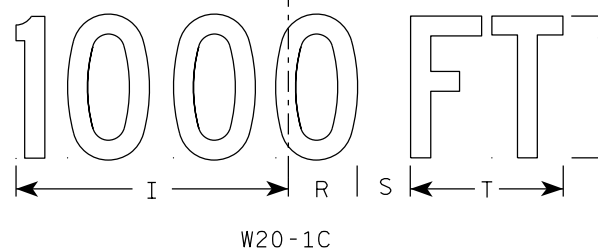
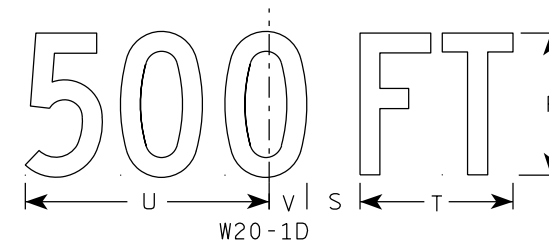
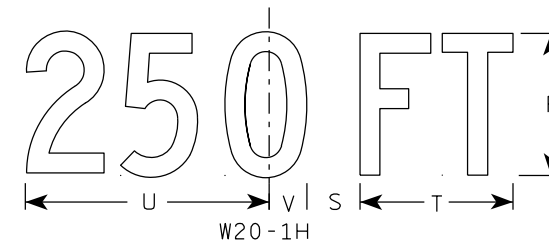
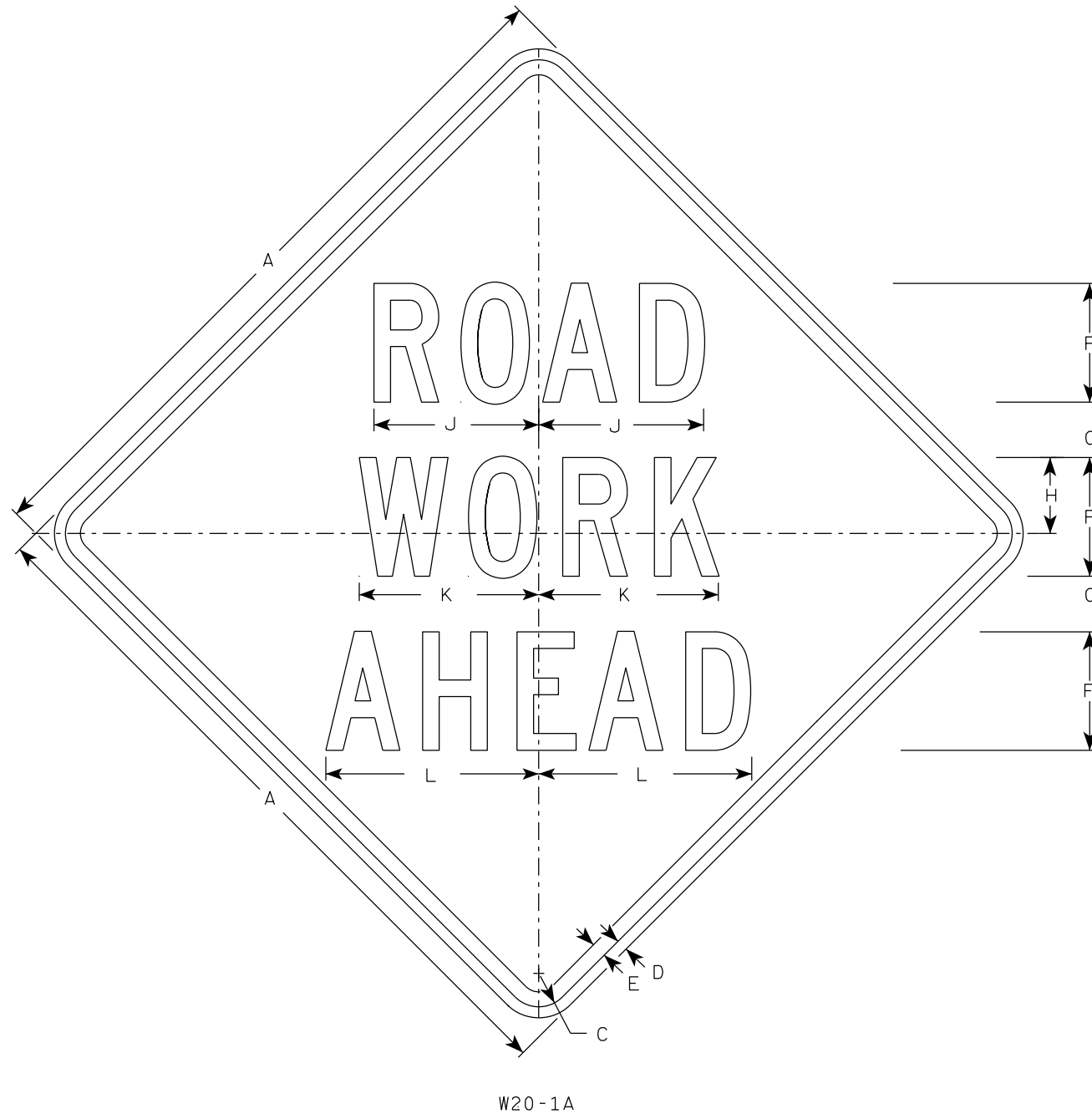
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 3/21/17 PLATE NO. W14-3.10

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



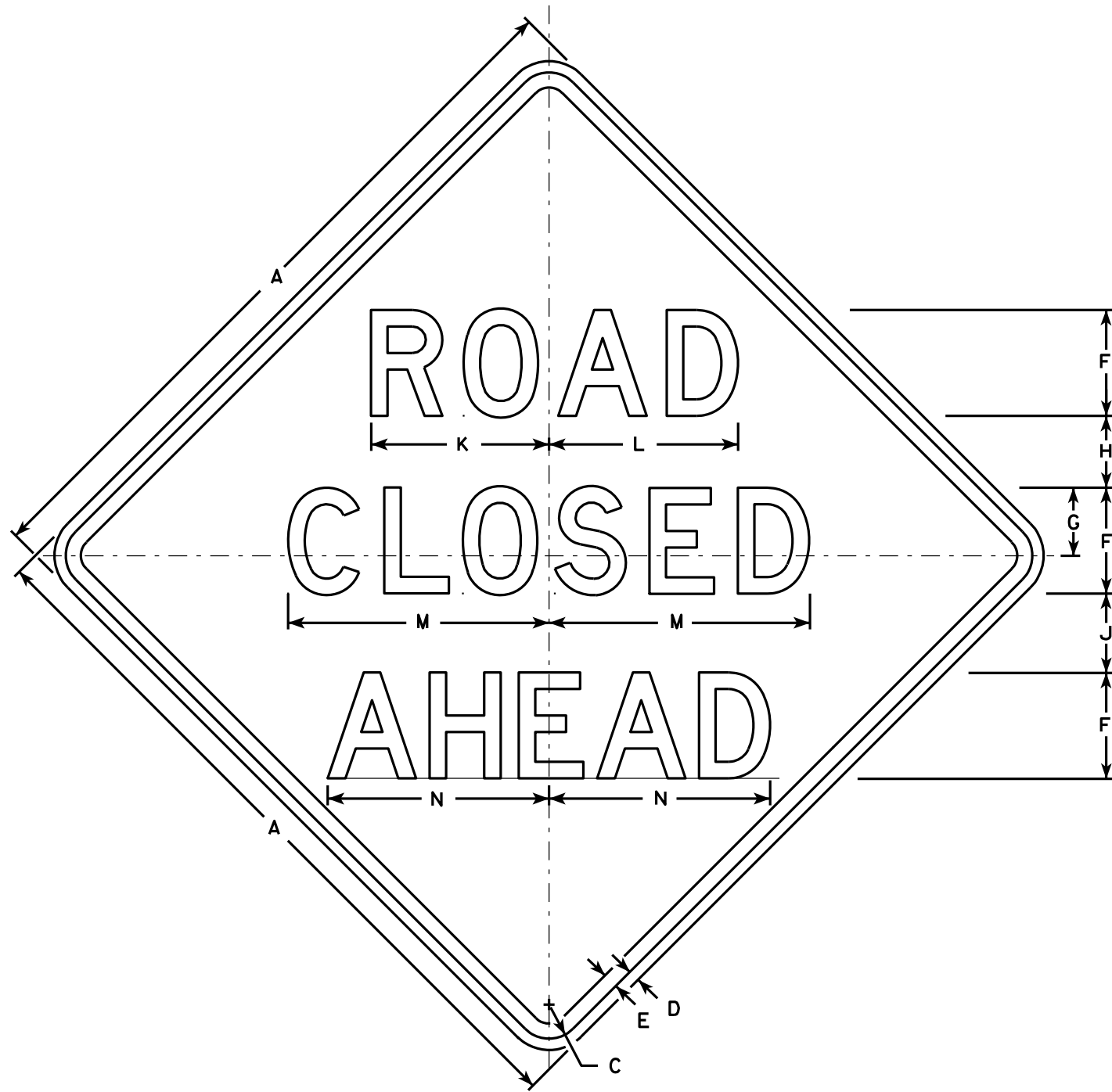
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	5	2 5/8	3 1/4	10 1/8	7	7 5/8	8 7/8	1 1/8	4 1/2	3 1/2	9	3 1/4	2 1/2	2 1/4	5 5/8	9	1 3/8	8	1 3/4	10 3/4	6	9.0
2S	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
2M	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
3	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
4	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0
5	48		2 1/4	3/4	1	8	3 3/4	5 1/8	15 3/8	11 1/8	12 1/8	14 3/8	1 5/8	6 7/8	5 3/8	13 7/8	4 3/8	3 7/8	3	8 5/8	13 3/4	2 1/8	11 7/8	2 3/4	16 3/8	9	16.0

STANDARD SIGN
W20-1A, B, C, D, E, F, G & H

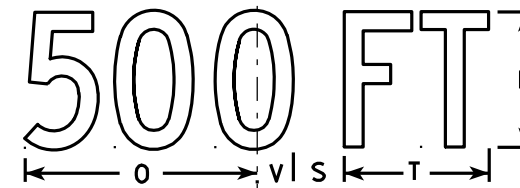
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

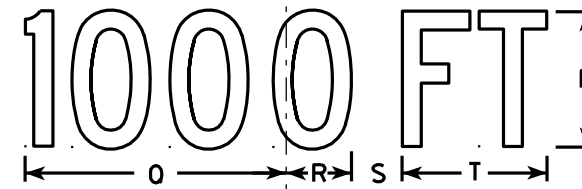
DATE 3/25/2020 PLATE NO. W20-1.11



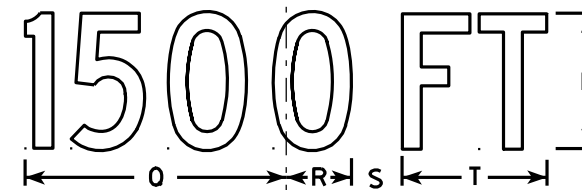
W20-3A



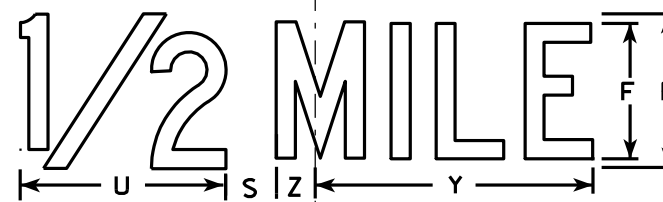
W20-3D



W20-3C



W20-3B



W20-3G



W20-3F

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - see note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Lines 1 and 2 are Series D.
Line 3 is Series D for AHEAD and Series C for all other distances.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	5	3 3/8	3 1/2	1 1/8	4	8 3/8	8 7/8	12 1/2	11	9	6	10 1/8	2 1/2	1 7/8	5 5/8	8	1 3/8	4 1/2	3 1/2	10 3/4	1 3/4	9.0
2S	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
2M	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
3	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
4	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
5	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0

STANDARD SIGN
W20-3A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 3/18/11 PLATE NO. W20-3.7

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E

EARTHWORK - MAINLINE

STATION	AREA (SF)				INCREMENTAL VOLUME (CY)									CUMULATIVE VOLUME (CY)									
	CUT	ROCK CUT	MARSH CUT	FILL	CUT NOTE 1	FILL NOTE 2	EXCAVATION ROCK	EXPANDED ROCK		EXCAVATION MARSH	REDUCED MARSH INFILL (0.6) NOTE 4	EXPANDED FILL		SELECT CRUSHED MATERIAL (1.5)	CUT (1.0) NOTE 1	FILL NOTE 2	EXCAVATION ROCK	EXPANDED ROCK		REDUCED MARSH INFILL (0.6) NOTE 4	SELECT CRUSHED MATERIAL (1.5)	FILL (1.25) NOTE 5	MASS ORDINATE NOTE 6
								(1.1) NOTE 3	(1.1)			(1.25) NOTE 5	(1.1) NOTE 3					(1.1)					
10+24	295	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
11+00	122	0	0	1	587	1	0	0	0	1	0	1	0	587	1	0	0	0	0	0	0	1	586
12+00	98	0	0	0	407	2	0	0	0	0	0	3	0	994	3	0	0	0	0	0	0	4	990
13+00	55	0	0	22	283	41	0	0	0	0	0	51	0	1277	44	0	0	0	0	0	0	55	1222
14+00	0	0	0	136	102	293	0	0	0	0	0	366	0	1379	337	0	0	0	0	0	0	421	958
15+00	0	0	0	221	0	661	0	0	0	0	0	826	0	1379	998	0	0	0	0	0	0	1247	132
16+00	0	0	0	188	0	757	0	0	0	0	0	946	0	1379	1755	0	0	0	0	0	0	2193	-814
17+00	0	0	0	97	0	528	0	0	0	0	0	660	0	1379	2283	0	0	0	0	0	0	2853	-1474
18+00	125	0	0	0	231	180	0	0	0	0	0	225	0	1610	2463	0	0	0	0	0	0	3078	-1468
19+00	228	0	0	0	654	0	0	0	0	0	0	0	0	2264	2463	0	0	0	0	0	0	3078	-814
20+00	286	0	0	0	952	0	0	0	0	0	0	0	0	3216	2463	0	0	0	0	0	0	3078	138
21+00	366	8	0	0	1207	0	15	17	0	0	0	-21	0	4423	2463	15	17	0	0	0	0	3057	1366
22+00	232	0	0	0	1107	0	15	17	0	0	0	-21	0	5530	2463	30	34	0	0	0	0	3036	2494
23+00	11	0	0	160	450	296	0	0	0	0	0	370	0	5980	2759	30	34	0	0	0	0	3406	2574
24+00	1	0	0	181	22	631	0	0	0	0	0	789	0	6002	3390	30	34	0	0	0	0	4195	1807
25+00	76	10	0	15	143	363	19	21	0	0	0	428	0	6145	3753	49	55	0	0	0	0	4623	1522
26+00	99	6	0	11	324	48	30	33	0	0	0	19	0	6469	3801	79	88	0	0	0	0	4642	1827
27+00	187	173	0	0	530	20	331	364	0	0	0	-430	0	6999	3821	410	452	0	0	0	0	4212	2787
28+00	150	90	0	0	624	0	487	536	0	0	0	-670	0	7623	3821	897	988	0	0	0	0	3542	4081
29+00	136	43	0	36	530	67	246	271	0	0	0	-255	0	8153	3888	1143	1259	0	0	0	0	3287	4866
30+00	91	8	0	7	420	80	94	103	0	0	0	-29	0	8573	3968	1237	1362	0	0	0	0	3258	5315
31+00	90	0	0	11	335	33	15	17	0	0	0	20	0	8908	4001	1252	1379	0	0	0	0	3278	5630
32+00	106	0	0	97	363	200	0	0	0	0	0	250	0	9271	4201	1252	1379	0	0	0	0	3528	5743
33+00	77	0	0	116	339	394	0	0	0	0	0	493	0	9610	4595	1252	1379	0	0	0	0	4021	5589
34+00	98	0	0	93	324	387	0	0	0	0	0	484	0	9934	4982	1252	1379	0	0	0	0	4505	5429
35+00	85	0	0	63	339	289	0	0	0	0	0	361	0	10273	5271	1252	1379	0	0	0	0	4866	5407
36+00	91	0	0	50	326	209	0	0	0	0	0	261	0	10599	5480	1252	1379	0	0	0	0	5127	5472
37+00	82	0	114	73	320	228	0	0	211	127	126	316	0	10919	5708	1252	1379	211	127	316	0	5253	5666
38+00	106	0	110	81	348	285	0	0	415	249	45	622	0	11267	5993	1252	1379	626	376	938	0	5298	5969
39+00	91	0	115	135	365	400	0	0	417	250	187	625	0	11632	6393	1252	1379	1043	626	1563	0	5485	6147
40+00	47	0	70	164	256	554	0	0	343	206	435	515	0	11888	6947	1252	1379	1386	832	2078	0	5920	5968
41+00	0	0	55	176	87	630	0	0	231	139	613	347	0	11975	7577	1252	1379	1617	971	2425	0	6533	5442
42+00	1	0	52	259	2	806	0	0	198	119	858	297	0	11977	8383	1252	1379	1815	1090	2722	0	7391	4586
43+00	0	0	51	211	2	870	0	0	191	114	945	287	0	11979	9253	1252	1379	2006	1204	3009	0	8336	3643
44+00	0	0	0	142	0	654	0	0	94	56	747	141	0	11979	9907	1252	1379	2100	1260	3150	0	9083	2896
45+00	77	0	0	71	143	394	0	0	0	0	0	493	0	12122	10301	1252	1379	2100	1260	3150	0	9576	2546
46+00	29	0	0	31	196	189	0	0	0	0	0	236	0	12318	10490	1252	1379	2100	1260	3150	0	9812	2506
47+00	27	0	0	31	104	115	0	0	0	0	0	144	0	12422	10605	1252	1379	2100	1260	3150	0	9956	2466
48+00	26	0	0	25	98	104	0	0	0	0	0	130	0	12520	10709	1252	1379	2100	1260	3150	0	10086	2434
49+00	26	0	0	27	96	96	0	0	0	0	0	120	0	12616	10805	1252	1379	2100	1260	3150	0	10206	2410
50+00	35	0	0	19	113	85	0	0	0	0	0	106	0	12729	10890	1252	1379	2100	1260	3150	0	10312	2417
51+00	0	0	0	28	65	87	0	0	0	0	0	109	0	12794	10977	1252	1379	2100	1260	3150	0	10421	2373
52+00	5	0	0	19	9	87	0	0	0	0	0	109	0	12803	11064	1252	1379	2100	1260	3150	0	10530	2273
53+00	11	0	0	10	30	54	0	0	0	0	0	68	0	12833	11118	1252	1379	2100	1260	3150	0	10598	2235
54+00	29	0	0	5	74	28	0	0	0	0	0	35	0	12907	11146	1252	1379	2100	1260	3150	0	10633	2274
55+00	78	0	0	6	198	20	0	0	0	0	0	25	0	13105	11166	1252	1379	2100	1260	3150	0	10658	2447
56+00	1	0	0	34	146	74	0	0	0	0	0	93	0	13251	11240	1252	1379	2100	1260	3150	0	10751	2500
57+00	0	0	0	38	2	133	0	0	0	0	0	166	0	13253	11373	1252	1379	2100	1260	3150	0	10917	2336
58+00	2	0	0	30	4	126	0	0	0	0	0	158	0	13257	11499	1252	1379	2100	1260	3150	0	11075	2182
59+00	0	0	0	25	4	102	0	0	0	0	0	128	0	13261	11601	1252	1379	2100	1260	3150	0	11203	2058
60+00	1	0	0	24	2	91	0	0	0	0	0	114	0	13263	11692	1252	1379	2100	1260	3150	0	11317	1946
61+00	101	0	0	20	189	81	0	0	0	0	0	101	0	13452	11773	1252	1379	2100	1260	3150	0	11418	2034
62+00	74	0	0	23	324	80	0	0	0	0	0	100	0	13776	11853	1252	1379	2100	1260	3150	0	11518	2258
63+00	76	0	0	45	278	126	0	0	0	0	0	158	0	14054	11979	1252	1379	2100	1260	3150	0	11676	2378
MAINLINE COLUMN SUBTOTALS =					14054	11979	1252	1379	2100	1260	11676	3150		14054	11979	1252	1379	2100	1260	3150	11676	2378	

NOTES:

1 - CUT	CUT INCLUDES SALVAGED/UNUSABLE MATERIAL
2 - FILL	DOES NOT INCLUDE UNUSABLE PAVEMENT EXCAVATION VOLUMES
3 - EXPANDED ROCK	EXPANDED ROCK THAT CAN BE USED AS FILL
4 - REDUCED MARSH IN FILL	REDUCED MARSH THAT CAN BE USED AS FILL
5 - FILL (25%)	FILL 25% = (FILL - EXPANDED ROCK (1.1) - REDUCED MARSH (0.6))*1.25
6 - MASS ORDINATE	CUT - FILL (25%)

NOTE: EARTHWORK QUANTITIES FOR THE MAINLINE INCLUDE THE QUANTITIES FOR ALIGNMENTS 'K', 'L', 'M', 'O', 'P', 'Q', AND 'R'-LINES.

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EARTHWORK - MAINLINE CONTINUED

STATION	AREA (SF)				INCREMENTAL VOLUME (CY)								CUMULATIVE VOLUME (CY)								
	CUT	ROCK CUT	MARSH CUT	FILL	CUT NOTE 1	FILL NOTE 2	EXCAVATION ROCK	EXPANDED ROCK		REDUCED MARSH INFILL (0.6) NOTE 4	EXPANDED FILL (1.25) NOTE 5	SELECT CRUSHED MATERIAL (1.5)	CUT (1.0) NOTE 1	FILL NOTE 2	EXCAVATION ROCK	EXPANDED ROCK		REDUCED MARSH- INFILL (0.6) NOTE 4	SELECT CRUSHED MATERIAL (1.5) NOTE 5	FILL (1.25) NOTE 5	MASS ORDINATE NOTE 6
								(1.1) NOTE 3	EXCAVATION MARSH							(1.1) NOTE 3	EXCAVATION MARSH				
64+00	68	0	0	45	267	167	0	0	0	0	209	0	14321	12146	1252	1379	2100	1260	3150	1885	2436
65+00	45	0	0	59	209	193	0	0	0	0	241	0	14530	12339	1252	1379	2100	1260	3150	1216	2404
66+00	23	0	0	83	128	283	0	0	0	0	329	0	14656	12502	1252	1379	2100	1260	3150	2455	2201
67+00	37	0	0	63	111	270	0	0	0	0	338	0	14787	12672	1252	1379	2100	1260	3150	2793	1974
68+00	51	0	0	47	163	204	0	0	0	0	255	0	14930	13076	1252	1379	2100	1260	3150	3048	1882
69+00	70	0	0	34	224	150	0	0	0	0	188	0	15154	13226	1252	1379	2100	1260	3150	3236	1918
70+00	72	0	0	32	263	122	0	0	0	0	153	0	15417	13348	1252	1379	2100	1260	3150	3389	2028
71+00	96	0	0	26	311	107	0	0	0	0	134	0	15728	13455	1252	1379	2100	1260	3150	3523	2205
72+00	134	0	0	22	425	89	0	0	0	0	111	0	16154	13544	1252	1379	2100	1260	3150	3634	2520
73+00	153	0	0	19	531	76	0	0	0	0	95	0	16685	13623	1252	1379	2100	1260	3150	3729	2956
74+00	110	0	0	41	487	111	0	0	0	0	139	0	17172	13731	1252	1379	2100	1260	3150	3868	3304
75+00	61	0	0	69	317	204	0	0	0	0	255	0	17489	13935	1252	1379	2100	1260	3150	4123	3366
76+00	52	0	0	50	209	220	0	0	0	0	275	0	17898	14155	1252	1379	2100	1260	3150	4398	3300
77+00	39	0	0	51	169	187	0	0	0	0	234	0	17857	14342	1252	1379	2100	1260	3150	4632	3235
78+00	51	0	0	85	167	252	0	0	0	0	315	0	18034	14594	1252	1379	2100	1260	3150	4947	3087
79+00	92	0	0	107	265	356	0	0	0	0	445	0	18299	14953	1252	1379	2100	1260	3150	5392	2907
80+00	153	0	0	136	454	450	0	0	0	0	563	0	18753	15403	1252	1379	2100	1260	3150	5955	2798
81+00	128	0	0	274	483	758	0	0	0	0	949	0	19236	16159	1252	1379	2100	1260	3150	6834	2332
82+00	124	0	0	41	430	583	0	0	0	0	729	0	19656	16742	1252	1379	2100	1260	3150	7633	2033
83+00	48	0	0	9	319	93	0	0	0	0	116	0	19985	16835	1252	1379	2100	1260	3150	7749	2236
84+00	5	0	0	80	98	165	0	0	0	0	206	0	20063	17003	1252	1379	2100	1260	3150	7855	2128
85+00	5	0	0	45	19	231	0	0	0	0	289	0	20102	17231	1252	1379	2100	1260	3150	8244	1858
86+00	0	0	0	8	9	98	0	0	0	0	123	0	20111	17329	1252	1379	2100	1260	3150	8367	1744
87+00	6	0	0	26	11	63	0	0	0	0	79	0	20122	17392	1252	1379	2100	1260	3150	8446	1676
88+00	36	0	0	33	78	109	0	0	0	0	136	0	20200	17501	1252	1379	2100	1260	3150	8582	1618
89+00	53	0	0	33	165	122	0	0	0	0	153	0	20365	17623	1252	1379	2100	1260	3150	8735	1630
90+00	56	0	0	53	202	159	0	0	0	0	199	0	20587	17782	1252	1379	2100	1260	3150	8934	1633
91+00	29	0	0	87	157	259	0	0	0	0	324	0	20724	18041	1252	1379	2100	1260	3150	9258	1466
92+00	7	0	0	101	67	348	0	0	0	0	435	0	20791	18389	1252	1379	2100	1260	3150	9693	1098
93+00	1	0	0	101	15	374	0	0	0	0	468	0	20806	18763	1252	1379	2100	1260	3150	20161	645
94+00	9	0	0	54	19	287	0	0	0	0	359	0	20825	19053	1252	1379	2100	1260	3150	20520	305
95+00	26	0	0	35	65	165	0	0	0	0	206	0	20890	19215	1252	1379	2100	1260	3150	20726	164
96+00	48	0	0	26	137	113	0	0	0	0	141	0	21027	19328	1252	1379	2100	1260	3150	20867	160
97+00	28	0	0	24	141	93	0	0	0	0	116	0	21168	19421	1252	1379	2100	1260	3150	20983	185
98+00	21	0	0	26	91	93	0	0	0	0	116	0	21259	19514	1252	1379	2100	1260	3150	21039	160
99+00	6	0	0	57	50	154	0	0	0	0	193	0	21309	19668	1252	1379	2100	1260	3150	21292	17
100+00	0	0	0	139	11	383	0	0	0	0	454	0	21320	20031	1252	1379	2100	1260	3150	21746	-426
101+00	0	0	0	132	0	502	0	0	0	0	628	0	21320	20533	1252	1379	2100	1260	3150	22374	-1054
102+00	1	0	0	72	2	378	0	0	0	0	473	0	21322	20911	1252	1379	2100	1260	3150	22847	-1525
103+00	1	0	0	46	4	219	0	0	0	0	274	0	21326	21133	1252	1379	2100	1260	3150	23121	-1795
104+00	1	0	0	41	4	161	0	0	0	0	201	0	21330	21291	1252	1379	2100	1260	3150	23322	-1992
105+00	5	0	0	38	11	146	0	0	0	0	183	0	21341	21437	1252	1379	2100	1260	3150	23535	-2164
106+00	10	0	0	40	28	144	0	0	0	0	180	0	21369	21581	1252	1379	2100	1260	3150	23695	-2316
107+00	15	0	0	46	46	159	0	0	0	0	199	0	21415	21740	1252	1379	2100	1260	3150	23884	-2469
108+00	30	0	0	63	93	202	0	0	0	0	253	0	21498	21942	1252	1379	2100	1260	3150	24137	-2639
109+00	0	0	0	183	56	419	0	0	0	0	524	0	21554	22361	1252	1379	2100	1260	3150	24661	-3107
110+00	0	0	0	141	0	563	0	0	0	0	704	0	21554	22924	1252	1379	2100	1260	3150	25365	-3811
111+00	0	0	0	39	0	333	0	0	0	0	416	0	21554	23257	1252	1379	2100	1260	3150	25781	-4227
112+00	0	0	0	35	0	317	0	0	0	0	171	0	21554	23394	1252	1379	2100	1260	3150	25852	-4398
113+00	0	0	0	71	0	196	0	0	0	0	245	0	21554	23593	1252	1379	2100	1260	3150	26197	-4643
114+00	0	0	0	80	0	280	0	0	0	0	350	0	21554	23873	1252	1379	2100	1260	3150	28547	-4993
115+00	30	0	0	73	56	283	0	0	0	0	354	0	21610	24153	1252	1379	2100	1260	3150	28931	-5291
116+00	26	0	0	73	104	270	0	0	0	0	338	0	21714	24423	1252	1379	2100	1260	3150	27239	-5525
117+00	3	0	0	106	54	331	0	0	0	0	414	0	21768	24754	1252	1379	2100	1260	3150	27653	-5885
MAINLINE COLUMN SUBTOTALS =					7714	12775	0	0	0	0	15977	0	21758	24754	1252	1379	2100	1260	3150	27653	-5885

NOTES:

- 1 - CUT
 - 2 - FILL
 - 3 - EXPANDED ROCK
 - 4 - REDUCED MARSH IN FILL
 - 5 - FILL (25%)
 - 6 - MASS ORDINATE
- CUT INCLUDES SALVAGED/UNUSABLE MATERIAL
 DOES NOT INCLUDE UNUSABLE PAVEMENT EXCAVATION VOLUMES
 EXPANDED ROCK THAT CAN BE USED AS FILL
 REDUCED MARSH THAT CAN BE USED AS FILL
 FILL 25% = (FILL - EXPANDED ROCK (1.1) - REDUCED MARSH (0.6))*1.25
 CUT - FILL (25%)

NOTE: EARTHWORK QUANTITIES FOR THE MAINLINE INCLUDE THE QUANTITIES FOR ALIGNMENTS 'K', 'L', 'M', 'O', 'P', 'Q', AND 'R'-LINES.

EARTHWORK - MAINLINE CONTINUED

STATION	AREA (SF)				INCREMENTAL VOLUME (CY)							CUMULATIVE VOLUME (CY)								
	CUT	ROCK CUT	MARSH CUT	FILL	CUT NOTE 1	FILL NOTE 2	EXCAVATION ROCK	EXPANDED ROCK	REDUCED MARSH	EXPANDED FILL	SELECT CRUSHED MATERIAL	CUT (1.0) NOTE 1	FILL NOTE 2	EXCAVATION ROCK	EXPANDED ROCK	REDUCED MARSH INFILL	SELECT CRUSHED MATERIAL	FILL (1.25) NOTE 5	MASS ORDINATE NOTE 6	
								(1.1) NOTE 3	INFILL (0.6) NOTE 4	(1.25) NOTE 5	(1.1) NOTE 3				(0.6) NOTE 4	(1.5) NOTE 5				
118+00	9	0	0	105	22	391	0	0	0	489	0	21790	25145	1252	1379	2100	1260	3150	28142	-6352
119+00	4	0	0	79	24	341	0	0	0	426	0	21814	25486	1252	1379	2100	1260	3150	28568	-6754
120+00	14	0	0	53	33	244	0	0	0	305	0	21847	25730	1252	1379	2100	1260	3150	28873	-7026
121+00	5	0	0	54	35	198	0	0	0	248	0	21882	25928	1252	1379	2100	1260	3150	29121	-7239
122+00	3	0	0	60	15	211	0	0	0	264	0	21897	26139	1252	1379	2100	1260	3150	29385	-7488
123+00	3	0	0	40	11	185	0	0	0	231	0	21908	26324	1252	1379	2100	1260	3150	29616	-7708
124+00	1	0	0	42	7	152	0	0	0	190	0	21915	26476	1252	1379	2100	1260	3150	29806	-7891
125+00	23	0	0	17	44	109	0	0	0	136	0	21959	26585	1252	1379	2100	1260	3150	29942	-7983
126+00	57	0	0	14	148	57	0	0	0	71	0	22107	26642	1252	1379	2100	1260	3150	30013	-7906
127+00	33	0	0	14	167	52	0	0	0	65	0	22274	26694	1252	1379	2100	1260	3150	30078	-7804
128+00	22	0	0	17	102	57	0	0	0	71	0	22376	26751	1252	1379	2100	1260	3150	30149	-7773
129+00	8	0	0	17	56	63	0	0	0	79	0	22432	26814	1252	1379	2100	1260	3150	30228	-7796
130+00	9	0	0	62	31	146	0	0	0	183	0	22463	26960	1252	1379	2100	1260	3150	30411	-7948
131+00	6	0	0	93	28	287	0	0	0	359	0	22491	27247	1252	1379	2100	1260	3150	30770	-8279
132+00	5	0	0	114	20	383	0	0	0	479	0	22511	27630	1252	1379	2100	1260	3150	31249	-8738
133+00	23	0	0	75	52	350	0	0	0	438	0	22563	27980	1252	1379	2100	1260	3150	31687	-9124
134+00	46	0	0	59	128	248	0	0	0	310	0	22691	28228	1252	1379	2100	1260	3150	31997	-9306
135+00	71	0	0	45	217	193	0	0	0	241	0	22908	28421	1252	1379	2100	1260	3150	32238	-9330
136+00	52	0	0	87	228	244	0	0	0	305	0	23136	28665	1252	1379	2100	1260	3150	32543	-9407
137+00	20	0	0	95	133	337	0	0	0	421	0	23269	29002	1252	1379	2100	1260	3150	32964	-9695
138+00	9	0	0	45	54	259	0	0	0	324	0	23323	29261	1252	1379	2100	1260	3150	33288	-9965
139+00	3	0	0	34	22	146	0	0	0	183	0	23345	29407	1252	1379	2100	1260	3150	33471	-10126
140+00	21	0	0	21	44	102	0	0	0	128	0	23389	29509	1252	1379	2100	1260	3150	33599	-10210
141+00	43	0	0	29	119	93	0	0	0	116	0	23508	29602	1252	1379	2100	1260	3150	33715	-10207
142+00	12	0	0	70	102	183	0	0	0	229	0	23610	29785	1252	1379	2100	1260	3150	33944	-10334
143+00	5	0	0	70	31	259	0	0	0	324	0	23641	30044	1252	1379	2100	1260	3150	34268	-10627
144+00	17	0	0	49	41	220	0	0	0	275	0	23682	30264	1252	1379	2100	1260	3150	34543	-10861
145+00	0	0	0	52	31	187	0	0	0	234	0	23713	30451	1252	1379	2100	1260	3150	34777	-11064
146+00	0	0	0	80	0	244	0	0	0	305	0	23713	30695	1252	1379	2100	1260	3150	35082	-11369
147+00	5	0	0	62	9	263	0	0	0	329	0	23722	30958	1252	1379	2100	1260	3150	35411	-11689
148+00	29	0	0	63	63	231	0	0	0	289	0	23785	31189	1252	1379	2100	1260	3150	35700	-11915
149+00	32	0	0	64	113	235	0	0	0	294	0	23898	31424	1252	1379	2100	1260	3150	35994	-12096
150+00	11	0	0	53	80	217	0	0	0	271	0	23978	31641	1252	1379	2100	1260	3150	36265	-12287
151+00	6	0	0	41	31	174	0	0	0	218	0	24009	31815	1252	1379	2100	1260	3150	36483	-12474
152+00	2	0	0	20	15	113	0	0	0	141	0	24024	31928	1252	1379	2100	1260	3150	36624	-12600
153+00	0	0	0	41	4	113	0	0	0	141	0	24028	32041	1252	1379	2100	1260	3150	36765	-12737
154+00	5	0	0	82	9	228	0	0	0	285	0	24037	32269	1252	1379	2100	1260	3150	37050	-13013
155+00	6	0	0	151	20	431	0	0	0	539	0	24057	32700	1252	1379	2100	1260	3150	37589	-13532
156+00	5	0	0	78	20	424	0	0	0	530	0	24077	33124	1252	1379	2100	1260	3150	38119	-14042
157+00	7	0	0	55	22	246	0	0	0	308	0	24099	33370	1252	1379	2100	1260	3150	38427	-14328
158+00	21	0	0	42	52	180	0	0	0	225	0	24151	33550	1252	1379	2100	1260	3150	38652	-14501
159+00	7	0	0	35	52	143	0	0	0	179	0	24203	33693	1252	1379	2100	1260	3150	38831	-14628
160+00	3	0	0	32	19	124	0	0	0	155	0	24222	33817	1252	1379	2100	1260	3150	38986	-14764
161+00	4	0	0	28	13	111	0	0	0	139	0	24235	33928	1252	1379	2100	1260	3150	39125	-14890
162+00	16	0	0	53	37	150	0	0	0	188	0	24272	34078	1252	1379	2100	1260	3150	39313	-15041
163+00	39	0	0	39	102	170	0	0	0	213	0	24374	34248	1252	1379	2100	1260	3150	39526	-15152
164+00	15	0	0	36	100	139	0	0	0	174	0	24474	34387	1252	1379	2100	1260	3150	39700	-15226
165+00	0	0	0	48	28	156	0	0	0	195	0	24502	34543	1252	1379	2100	1260	3150	39895	-15393
166+00	4	0	0	56	7	193	0	0	0	241	0	24509	34736	1252	1379	2100	1260	3150	40136	-15627
167+00	8	0	0	77	22	246	0	0	0	308	0	24531	34982	1252	1379	2100	1260	3150	40444	-15913
168+00	17	0	0	88	46	306	0	0	0	383	0	24577	35288	1252	1379	2100	1260	3150	40827	-16250
169+00	92	0	0	79	202	309	0	0	0	386	0	24779	35597	1252	1379	2100	1260	3150	41213	-16434
170+00	156	0	0	0	459	146	0	0	0	183	0	25238	35743	1252	1379	2100	1260	3150	41396	-16158
171+00	221	0	0	0	698	0	0	0	0	0	0	25936	35743	1252	1379	2100	1260	3150	41396	-15460
MAINLINE COLUMN SUBTOTALS =				4168	10989	0	0	0	0	13743	0	25936	35743	1252	1379	2100	1260	3150	41396	-15460

NOTES:
 1 - CUT CUT INCLUDES SALVAGED/UNUSABLE MATERIAL
 2 - FILL DOES NOT INCLUDE UNUSABLE PAVEMENT EXCAVATION VOLUMES
 3 - EXPANDED ROCK EXPANDED ROCK THAT CAN BE USED AS FILL
 4 - REDUCED MARSH IN FILL REDUCED MARSH THAT CAN BE USED AS FILL
 5 - FILL (25%) FILL 25% = (FILL - EXPANDED ROCK (1.1) - REDUCED MARSH (0.6))*1.25
 6 - MASS ORDINATE CUT - FILL (25%)
 NOTE: EARTHWORK QUANTITIES FOR THE MAINLINE INCLUDE THE QUANTITIES FOR ALIGNMENTS 'K', 'L', 'M', 'O', 'P', 'Q', AND 'R'-LINES.

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EARTHWORK - MAINLINE CONTINUED

STATION	AREA (SF)				INCREMENTAL VOLUME (CY)								CUMULATIVE VOLUME (CY)									
	CUT	ROCK CUT	MARSH CUT	FILL	CUT NOTE 1	FILL NOTE 2	EXCAVATION ROCK	EXPANDED		REDUCED MARSH INFILL (0.6) NOTE 4	EXPANDED		SELECT CRUSHED MATERIAL (1.5)	CUT NOTE 1	FILL NOTE 2	EXCAVATION ROCK	EXPANDED		REDUCED MARSH INFILL (0.6) NOTE 4	SELECT CRUSHED MATERIAL (1.5)	FILL (1.25) NOTE 5	MASS ORDINATE NOTE 6
								ROCK (1.1) NOTE 3	EXCAVATION MARSH		FILL (1.25) NOTE 5	ROCK (1.1) NOTE 3					EXCAVATION MARSH					
172+00	298	0	0	0	961	0	0	0	0	0	0	0	0	26897	35743	1252	1379	2100	1260	3150	41396	-14499
173+00	149	0	0	24	828	44	0	0	0	0	55	0	0	27725	35787	1252	1379	2100	1260	3150	41451	-13726
174+00	7	0	0	59	289	154	0	0	0	0	193	0	0	28014	35941	1252	1379	2100	1260	3150	41644	-13630
175+00	0	0	0	191	13	463	0	0	0	0	579	0	0	28027	36404	1252	1379	2100	1260	3150	42223	-14196
176+00	0	0	0	281	0	874	0	0	0	0	1093	0	0	28027	37278	1252	1379	2100	1260	3150	43316	-15289
177+00	0	0	0	230	0	946	0	0	0	0	1183	0	0	28027	38224	1252	1379	2100	1260	3150	44499	-16472
178+00	18	0	0	58	33	533	0	0	0	0	666	0	0	28060	38757	1252	1379	2100	1260	3150	45165	-17105
179+00	53	0	0	12	131	130	0	0	0	0	163	0	0	28191	38887	1252	1379	2100	1260	3150	45328	-17137
180+00	15	0	0	51	126	117	0	0	0	0	146	0	0	28317	39004	1252	1379	2100	1260	3150	45474	-17157
181+00	3	0	0	85	33	252	0	0	0	0	315	0	0	28350	39256	1252	1379	2100	1260	3150	45789	-17439
182+00	2	0	0	156	9	446	0	0	0	0	558	0	0	28359	39702	1252	1379	2100	1260	3150	46347	-17988
183+00	0	0	0	112	4	496	0	0	0	0	620	0	0	28363	40198	1252	1379	2100	1260	3150	46967	-18604
184+00	7	0	0	49	13	298	0	0	0	0	373	0	0	28376	40496	1252	1379	2100	1260	3150	47340	-18964
185+00	35	0	0	18	78	124	0	0	0	0	155	0	0	28454	40620	1252	1379	2100	1260	3150	47495	-19041
186+00	37	0	0	14	133	59	0	0	0	0	74	0	0	28587	40679	1252	1379	2100	1260	3150	47569	-18982
187+00	39	0	0	44	141	107	0	0	0	0	134	0	0	28728	40786	1252	1379	2100	1260	3150	47703	-18975
188+00	17	0	0	174	104	404	0	0	0	0	505	0	0	28832	41190	1252	1379	2100	1260	3150	48208	-19376
189+00	12	0	0	267	54	817	0	0	0	0	1021	0	0	28886	42007	1252	1379	2100	1260	3150	49229	-20343
190+00	18	0	0	137	56	748	0	0	0	0	935	0	0	28942	42755	1252	1379	2100	1260	3150	50164	-21222
191+00	20	0	0	63	70	370	0	0	0	0	463	0	0	29012	43125	1252	1379	2100	1260	3150	50627	-21615
192+00	3	0	0	48	43	206	0	0	0	0	258	0	0	29055	43331	1252	1379	2100	1260	3150	50885	-21830
193+00	4	0	0	30	13	144	0	0	0	0	180	0	0	29068	43475	1252	1379	2100	1260	3150	51065	-21997
194+00	8	0	0	24	22	100	0	0	0	0	125	0	0	29090	43575	1252	1379	2100	1260	3150	51190	-22100
195+00	27	0	0	30	65	100	0	0	0	0	125	0	0	29155	43675	1252	1379	2100	1260	3150	51315	-22160
196+00	20	0	0	49	87	146	0	0	0	0	183	0	0	29242	43821	1252	1379	2100	1260	3150	51498	-22256
197+00	16	0	0	52	67	187	0	0	0	0	234	0	0	29309	44008	1252	1379	2100	1260	3150	51732	-22423
198+00	6	0	0	97	41	276	0	0	0	0	345	0	0	29350	44284	1252	1379	2100	1260	3150	52077	-22727
199+00	11	0	0	98	31	361	0	0	0	0	451	0	0	29381	44645	1252	1379	2100	1260	3150	52528	-23147
200+00	24	0	0	73	65	317	0	0	0	0	396	0	0	29446	44962	1252	1379	2100	1260	3150	52924	-23478
201+00	18	0	0	74	78	272	0	0	0	0	340	0	0	29524	45234	1252	1379	2100	1260	3150	53264	-23740
202+00	18	0	0	51	67	231	0	0	0	0	289	0	0	29591	45465	1252	1379	2100	1260	3150	53553	-23962
203+00	30	0	0	8	89	109	0	0	0	0	136	0	0	29680	45574	1252	1379	2100	1260	3150	53689	-24009
204+00	16	0	0	34	85	78	0	0	0	0	98	0	0	29765	45652	1252	1379	2100	1260	3150	53787	-24022
205+00	1	0	0	81	31	213	0	0	0	0	266	0	0	29796	45865	1252	1379	2100	1260	3150	54053	-24257
206+00	2	0	0	100	6	335	0	0	0	0	419	0	0	29802	46200	1252	1379	2100	1260	3150	54472	-24670
207+00	32	0	0	40	63	259	0	0	0	0	324	0	0	29865	46459	1252	1379	2100	1260	3150	54796	-24931
208+00	48	0	0	17	148	106	0	0	0	0	133	0	0	30013	46565	1252	1379	2100	1260	3150	54929	-24916
209+00	41	0	0	18	165	65	0	0	0	0	81	0	0	30178	46630	1252	1379	2100	1260	3150	55010	-24832
210+00	30	0	0	20	131	70	0	0	0	0	88	0	0	30309	46700	1252	1379	2100	1260	3150	55098	-24789
211+00	1	0	0	56	57	141	0	0	0	0	176	0	0	30366	46841	1252	1379	2100	1260	3150	55274	-24908
212+00	16	0	0	164	31	407	0	0	0	0	509	0	0	30397	47248	1252	1379	2100	1260	3150	55783	-25386
213+00	9	0	0	358	46	967	0	0	0	0	1209	0	0	30443	48215	1252	1379	2100	1260	3150	56992	-26549
214+00	0	0	0	338	17	1289	0	0	0	0	1611	0	0	30460	49504	1252	1379	2100	1260	3150	58603	-28143
215+00	19	0	0	146	35	896	0	0	0	0	1120	0	0	30495	50400	1252	1379	2100	1260	3150	59723	-29228
216+00	85	0	0	53	193	369	0	0	0	0	461	0	0	30688	50769	1252	1379	2100	1260	3150	60184	-29496
217+00	58	0	0	72	265	231	0	0	0	0	289	0	0	30953	51000	1252	1379	2100	1260	3150	60473	-29520
218+00	34	0	0	111	170	339	0	0	0	0	424	0	0	31123	51339	1252	1379	2100	1260	3150	60897	-29774
219+00	31	0	0	71	120	337	0	0	0	0	421	0	0	31243	51676	1252	1379	2100	1260	3150	61318	-30075
220+00	52	0	0	50	154	224	0	0	0	0	280	0	0	31397	51900	1252	1379	2100	1260	3150	61598	-30201
221+00	192	19	0	8	452	107	35	38	0	0	86	0	0	31849	52007	1287	1417	2100	1260	3150	61684	-29835
222+00	323	16	0	0	954	15	65	71	0	0	-70	0	0	32803	52022	1352	1488	2100	1260	3150	61614	-28811
223+00	345	46	0	0	1237	0	115	126	0	0	-158	0	0	34040	52022	1467	1614	2100	1260	3150	61456	-27416
224+00	253	13	0	0	1107	0	109	120	0	0	-150	0	0	35147	52022	1576	1734	2100	1260	3150	61306	-26159
225+00	105	0	0	29	663	54	24	26	0	0	35	0	0	35810	52076	1600	1760	2100	1260	3150	61341	-25531
MAINLINE COLUMN SUBTOTALS =				9874	16333	348	381	0	0	19945	0	35810	52076	1600	1760	2100	1260	3150	61341	-25531		

NOTES:

1 - CUT	CUT INCLUDES SALVAGED/UNUSABLE MATERIAL
2 - FILL	DOES NOT INCLUDE UNUSABLE PAVEMENT EXCAVATION VOLUMES
3 - EXPANDED ROCK	EXPANDED ROCK THAT CAN BE USED AS FILL
4 - REDUCED MARSH IN FILL	REDUCED MARSH THAT CAN BE USED AS FILL
5 - FILL (25%)	FILL 25% = (FILL - EXPANDED ROCK (1.1) - REDUCED MARSH (0.6))*1.25
6 - MASS ORDINATE	CUT - FILL (25%)

NOTE: EARTHWORK QUANTITIES FOR THE MAINLINE INCLUDE THE QUANTITIES FOR ALIGNMENTS 'K', 'L', 'M', 'O', 'P', 'Q', AND 'R'-LINES.

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EARTHWORK - MAINLINE CONTINUED

STATION	AREA (SF)				INCREMENTAL VOLUME (CY)							CUMULATIVE VOLUME (CY)										
	CUT	ROCK CUT	MARSH CUT	FILL	CUT NOTE 1	FILL NOTE 2	EXCAVATION ROCK	EXPANDED	REDUCED	EXPANDED	SELECT	CUT (1.0) NOTE 1	FILL NOTE 2	EXCAVATION ROCK	EXPANDED	REDUCED	SELECT	FILL (1.25) NOTE 5	MASS ORDINATE NOTE 6			
								ROCK (1.1) NOTE 3	MARSH INFILL (0.6) NOTE 4	FILL (1.25) NOTE 5	CRUSHED MATERIAL (1.5)				ROCK (1.1) NOTE 3	MARSH INFILL (0.6) NOTE 4	CRUSHED MATERIAL (1.5)					
226+00	68	0	0	136	320	306	0	0	0	0	383	0	36130	52382	1600	1760	2100	1260	3150	61724	-25594	
227+00	32	0	0	173	185	572	0	0	0	0	715	0	36315	52954	1600	1760	2100	1260	3150	62439	-26124	
228+00	23	0	0	122	102	546	0	0	0	0	683	0	36417	53500	1600	1760	2100	1260	3150	63122	-26705	
229+00	27	0	0	75	93	365	0	0	0	0	456	0	36510	53865	1600	1760	2100	1260	3150	63578	-27068	
230+00	8	0	0	48	65	228	0	0	0	0	285	0	36575	54093	1600	1760	2100	1260	3150	63863	-27288	
231+00	9	0	0	30	31	144	0	0	0	0	180	0	36606	54237	1600	1760	2100	1260	3150	64043	-27437	
232+00	3	0	0	32	22	115	0	0	0	0	144	0	36628	54352	1600	1760	2100	1260	3150	64187	-27559	
233+00	10	0	0	39	24	131	0	0	0	0	164	0	36652	54483	1600	1760	2100	1260	3150	64351	-27699	
234+00	10	0	0	33	37	133	0	0	0	0	166	0	36689	54616	1600	1760	2100	1260	3150	64517	-27828	
235+00	4	0	0	34	26	124	0	0	0	0	155	0	36715	54740	1600	1760	2100	1260	3150	64672	-27957	
236+00	7	0	0	46	20	148	0	0	0	0	185	0	36735	54888	1600	1760	2100	1260	3150	64857	-28122	
237+00	6	0	0	84	24	241	0	0	0	0	301	0	36759	55129	1600	1760	2100	1260	3150	65158	-28399	
238+00	20	0	0	64	48	274	0	0	0	0	343	0	36807	55403	1600	1760	2100	1260	3150	65501	-28694	
239+00	6	0	0	90	48	285	0	0	0	0	356	0	36855	55688	1600	1760	2100	1260	3150	65857	-29002	
240+00	10	0	0	59	30	276	0	0	0	0	345	0	36885	55964	1600	1760	2100	1260	3150	66202	-29317	
241+00	14	0	0	57	44	215	0	0	0	0	269	0	36929	56179	1600	1760	2100	1260	3150	66471	-29542	
242+00	12	0	0	46	48	191	0	0	0	0	239	0	36977	56370	1600	1760	2100	1260	3150	66710	-29733	
243+00	4	0	0	39	30	157	0	0	0	0	196	0	37007	56527	1600	1760	2100	1260	3150	66906	-29899	
244+00	0	0	0	45	7	156	0	0	0	0	195	0	37014	56683	1600	1760	2100	1260	3150	67101	-30087	
245+00	0	0	0	57	0	189	0	0	0	0	236	0	37014	56872	1600	1760	2100	1260	3150	67337	-30323	
246+00	2	0	0	50	4	198	0	0	0	0	248	0	37018	57070	1600	1760	2100	1260	3150	67585	-30567	
247+00	0	0	0	57	4	198	0	0	0	0	248	0	37022	57268	1600	1760	2100	1260	3150	67833	-30811	
248+00	1	0	0	101	2	293	0	0	0	0	366	0	37024	57561	1600	1760	2100	1260	3150	68199	-31175	
249+00	0	0	0	107	2	385	0	0	0	0	481	0	37026	57946	1600	1760	2100	1260	3150	68680	-31654	
250+00	0	0	0	109	0	400	0	0	0	0	500	0	37026	58346	1600	1760	2100	1260	3150	69180	-32154	
251+00	1	0	0	74	2	339	0	0	0	0	424	0	37028	58685	1600	1760	2100	1260	3150	69604	-32576	
252+00	1	0	0	29	4	191	0	0	0	0	239	0	37032	58876	1600	1760	2100	1260	3150	69843	-32811	
253+00	2	0	0	35	6	119	0	0	0	0	149	0	37038	58995	1600	1760	2100	1260	3150	69992	-32954	
254+00	6	0	0	22	15	106	0	0	0	0	133	0	37053	59101	1600	1760	2100	1260	3150	70125	-33072	
255+00	14	0	0	35	37	106	0	0	0	0	133	0	37090	59207	1600	1760	2100	1260	3150	70258	-33168	
256+00	7	0	0	51	39	159	0	0	0	0	199	0	37129	59366	1600	1760	2100	1260	3150	70457	-33328	
257+00	0	0	0	60	13	206	0	0	0	0	258	0	37142	59572	1600	1760	2100	1260	3150	70715	-33573	
258+00	60	0	0	174	111	433	0	0	0	0	541	0	37253	60005	1600	1760	2100	1260	3150	71256	-34003	
259+00	89	0	0	13	276	346	0	0	0	0	433	0	37529	60351	1600	1760	2100	1260	3150	71689	-34160	
260+00	92	0	0	18	335	57	0	0	0	0	71	0	37864	60408	1600	1760	2100	1260	3150	71760	-33896	
261+00	110	0	0	4	374	41	0	0	0	0	51	0	38238	60449	1600	1760	2100	1260	3150	71811	-33573	
262+00	81	0	0	9	354	24	0	0	0	0	30	0	38592	60473	1600	1760	2100	1260	3150	71841	-33249	
263+00	89	0	0	14	315	43	0	0	0	0	54	0	38907	60516	1600	1760	2100	1260	3150	71895	-32988	
264+00	80	0	0	36	313	93	0	0	0	0	116	0	39220	60609	1600	1760	2100	1260	3150	72011	-32791	
265+00	135	0	0	89	398	231	0	0	0	0	289	0	39618	60840	1600	1760	2100	1260	3150	72300	-32682	
265+24.10	273	0	0	0	182	40	0	0	0	0	50	0	39800	60880	1600	1760	2100	1260	3150	72350	-32550	
MAINLINE COLUMN SUBTOTALS =					3990	8804	0	0	0	0	11009	0										
MAINLINE COLUMN TOTALS =					39800	60880	1600	1760	2100	1260	72350	3150	0	39800	60880	1600	1760	2100	1260	3150	72350	-32550

NOTES:

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2 - FILL	DOES NOT INCLUDE UNUSABLE PAVEMENT EXCAVATION VOLUMES
3 - EXPANDED ROCK	EXPANDED ROCK THAT CAN BE USED AS FILL
4 - REDUCED MARSH IN FILL	REDUCED MARSH THAT CAN BE USED AS FILL
5 - FILL (25%)	FILL 25% = (FILL - EXPANDED ROCK (1.1) - REDUCED MARSH (0.6))*1.25
6 - MASS ORDINATE	CUT - FILL (25%)

NOTE: EARTHWORK QUANTITIES FOR THE MAINLINE INCLUDE THE QUANTITIES FOR ALIGNMENTS 'K', 'L', 'M', 'O', 'P', 'Q', AND 'R'-LINES.

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EARTHWORK - 'A'-LINE (STH 78)

STATION	AREA (SF)		INCREMENTAL VOLUME (CY)			CUMULATIVE VOLUME (CY)			
	CUT	FILL	CUT NOTE 1	FILL NOTE 2	FILL 1.25 NOTE 5	CUT 1.00 NOTE 1	FILL NOTE 2	FILL 1.25 NOTE 5	MASS ORDINATE NOTE 6
399'A+55	0	0	0	0	0	0	0	0	0
400'A+00	0	0	0	0	0	0	0	0	0
400'A+50	4	6	4	6	8	4	6	8	-4
401'A+00	98	4	94	9	11	98	15	19	79
401'A+50	83	1	168	5	6	266	20	25	241
402'A+00	68	1	140	2	3	406	22	28	378
402'A+50	21	0	82	1	1	488	23	29	459
403'A+00	21	0	39	0	0	527	23	29	498
403'A+50	20	0	38	0	0	565	23	29	536
404'A+00	46	2	61	2	3	626	25	32	594
404'A+50	44	3	83	5	6	709	30	38	671
405'A+00	35	4	73	6	8	782	36	46	736
405'A+50	43	11	72	14	17	854	50	63	791
406'A+00	46	0	82	10	12	936	60	75	861
406'A+50	27	0	69	0	0	1005	60	75	930
406'A+79.11	27	0	30	0	0	1035	60	75	960
'A' - LINE COLUMN SUBTOTALS =			1035	60	75	1035	60	75	960

EARTHWORK - 'D'-LINE (SHARED USE ACCESS)

STATION	AREA (SF)		INCREMENTAL VOLUME (CY)			CUMULATIVE VOLUME (CY)			
	CUT	FILL	CUT NOTE 1	FILL NOTE 2	FILL 1.25 NOTE 5	CUT 1.00 NOTE 1	FILL NOTE 2	FILL 1.25 NOTE 5	MASS ORDINATE NOTE 6
400'D+00	27	2	0	0	0	0	0	0	0
400'D+50	27	2	50	4	5	50	4	5	45
401'D+00	144	0	158	2	3	208	6	8	200
401'D+50	220	0	337	0	0	545	6	8	537
402'D+00	87	1	284	1	1	829	7	9	820
402'D+50	0	31	81	30	37	910	37	46	864
402'D+87.03	0	206	0	163	204	910	200	250	660
'D' - LINE COLUMN SUBTOTALS =			910	200	250	910	200	250	660

EARTHWORK - 'B'-LINE (MOUND STREET)

STATION	AREA (SF)		INCREMENTAL VOLUME (CY)			CUMULATIVE VOLUME (CY)			
	CUT	FILL	CUT NOTE 1	FILL NOTE 2	FILL 1.25 NOTE 5	CUT 1.00 NOTE 1	FILL NOTE 2	FILL 1.25 NOTE 5	MASS ORDINATE NOTE 6
399'B+62.87	0	0	0	0	0	0	0	0	0
399'B+92.87	40	10	22	6	8	22	6	8	14
400'B+00	40	10	11	3	4	33	9	12	21
400'B+50	104	5	132	15	18	165	24	30	135
401'B+00	138	8	224	13	16	389	37	46	343
401'B+50	167	7	282	14	17	671	51	63	608
402'B+00	231	7	369	13	16	1040	64	79	961
402'B+03.52	231	7	30	1	1	1070	65	80	990
'B' - LINE COLUMN SUBTOTALS =			1070	65	80	1070	65	80	990

EARTHWORK - 'E'-LINE (HORSESHOE BEND ROAD)

STATION	AREA (SF)		INCREMENTAL VOLUME (CY)			CUMULATIVE VOLUME (CY)			
	CUT	FILL	CUT NOTE 1	FILL NOTE 2	FILL 1.25 NOTE 5	CUT 1.00 NOTE 1	FILL NOTE 2	FILL 1.25 NOTE 5	MASS ORDINATE NOTE 6
401'E+88	111	0	0	0	0	0	0	0	0
402'E+00	111	0	49	0	0	49	0	0	49
402'E+50	108	5	205	5	6	254	5	6	248
403'E+00	77	0	173	5	7	427	10	13	414
403'E+24.08	131	14	93	5	7	520	15	20	500
'E' - LINE COLUMN SUBTOTALS =			520	15	20	520	15	20	500

EARTHWORK - 'C'-LINE (P.E.)

STATION	AREA (SF)		INCREMENTAL VOLUME (CY)			CUMULATIVE VOLUME (CY)			
	CUT	FILL	CUT NOTE 1	FILL NOTE 2	FILL 1.25 NOTE 5	CUT 1.00 NOTE 1	FILL NOTE 2	FILL 1.25 NOTE 5	MASS ORDINATE NOTE 6
400'C+00	60	0	0	0	0	0	0	0	0
400'C+50	249	0	291	0	0	291	0	0	291
401'C+00	245	0	457	0	0	748	0	0	748
401'C+50	75	10	296	9	11	1044	9	11	1033
401'C+68.23	3	23	26	11	14	1070	20	25	1045
'C' - LINE COLUMN SUBTOTALS =			1070	20	25	1070	20	25	1045

EARTHWORK - 'F'-LINE (WEST MOSCOW ROAD)

STATION	AREA (SF)		INCREMENTAL VOLUME (CY)			CUMULATIVE VOLUME (CY)			
	CUT	FILL	CUT NOTE 1	FILL NOTE 2	FILL 1.25 NOTE 5	CUT 1.00 NOTE 1	FILL NOTE 2	FILL 1.25 NOTE 5	MASS ORDINATE NOTE 6
400'F+16	87	0	0	0	0	0	0	0	0
400'F+50	38	8	79	5	6	79	5	6	73
401'F+00	17	17	51	23	30	130	28	36	94
401'F+50	28	13	42	28	36	172	56	72	100
402'F+00	46	13	69	24	30	241	80	102	139
402'F+20	46	13	34	10	13	275	90	115	160
'F' - LINE COLUMN SUBTOTALS =			275	90	115	275	90	115	160

NOTES:

1 - CUT	CUT INCLUDES SALVAGED/UNUSABLE MATERIAL
2 - FILL	DOES NOT INCLUDE UNUSABLE PAVEMENT EXCAVATION VOLUMES
3 - EXPANDED ROCK	EXPANDED ROCK THAT CAN BE USED AS FILL
4 - REDUCED MARSH IN FILL	REDUCED MARSH THAT CAN BE USED AS FILL
5 - FILL (25%)	FILL 25% = (FILL - EXPANDED ROCK (1.1) - REDUCED MARSH (0.6))*1.25
6 - MASS ORDINATE	CUT - FILL (25%)

NOTE: EARTHWORK QUANTITIES FOR THE MAINLINE INCLUDE THE QUANTITIES FOR ALIGNMENTS 'K', 'L', 'M', 'O', 'P', 'Q', AND 'R'-LINES.

EARTHWORK - 'G'-LINE (DRIVEWAY REALIGNMENT)

STATION	AREA (SF)		INCREMENTAL VOLUME (CY)			CUMULATIVE VOLUME (CY)			
	CUT	FILL	CUT NOTE 1	FILL NOTE 2	FILL 1.25 NOTE 5	CUT 1.00 NOTE 1	FILL NOTE 2	FILL 1.25 NOTE 5	MASS ORDINATE NOTE 6
400'G+15	61	0	0	0	0	0	0	0	0
400'G+50	61	0	79	0	0	79	0	0	79
401'G+00	63	1	115	1	1	194	1	1	193
401'G+50	76	2	129	3	4	323	4	5	318
402'G+00	54	3	120	5	6	443	9	11	432
402'G+50	63	4	108	6	8	551	15	19	532
403'G+00	43	3	98	6	8	649	21	27	622
403'G+50	29	5	67	7	9	716	28	36	680
404'G+00	34	5	58	9	10	774	37	46	728
404'G+50	12	9	43	13	15	817	50	61	756
405'G+00	20	4	30	12	15	847	62	76	771
405'G+50	51	0	66	3	4	913	65	80	833
406'G+00	61	0	104	0	0	1017	65	80	937
406'G+50	108	0	156	0	0	1173	65	80	1093
406'G+64.33	108	0	57	0	0	1230	65	80	1150
'G' - LINE COLUMN SUBTOTALS = 1230 65 80 1230 65 80 1150									

EARTHWORK - 'I'-LINE (EAST PECATONICA ROAD)

STATION	AREA (SF)		INCREMENTAL VOLUME (CY)			CUMULATIVE VOLUME (CY)			
	CUT	FILL	CUT NOTE 1	FILL NOTE 2	FILL 1.25 NOTE 5	CUT 1.00 NOTE 1	FILL NOTE 2	FILL 1.25 NOTE 5	MASS ORDINATE NOTE 6
402'I+50	38	22	0	0	0	0	0	0	0
403'I+00	38	22	70	41	51	70	41	51	19
403'I+50	28	231	61	234	293	131	275	344	-213
404'I+00	0	475	26	654	818	157	929	1162	-1005
404'I+50	0	866	0	1242	1553	157	2171	2715	-2558
405'I+00	0	1059	0	1782	2228	157	3953	4943	-4786
405'I+50	23	1118	21	2016	2520	178	5969	7463	-7285
406'I+00	25	1220	44	2165	2706	222	8134	10169	-9947
406'I+50	8	1066	31	2117	2646	253	10251	12815	-12562
407'I+00	9	890	16	1811	2264	269	12062	15079	-14810
407'I+50	11	835	19	1597	1996	288	13659	17075	-16787
408'I+00	9	669	19	1393	1741	307	15052	18816	-18509
408'I+50	8	523	16	1104	1380	323	16156	20196	-19873
409'I+00	0	245	7	711	888	330	16867	21084	-20754
409'I+24.87	43	0	20	113	141	350	16980	21225	-20875
'I' - LINE COLUMN SUBTOTALS = 350 16980 21225 350 16980 21225 -20875									

EARTHWORK - 'H'-LINE (McKENNA ROAD)

STATION	AREA (SF)		INCREMENTAL VOLUME (CY)			CUMULATIVE VOLUME (CY)			
	CUT	FILL	CUT NOTE 1	FILL NOTE 2	FILL 1.25 NOTE 5	CUT 1.00 NOTE 1	FILL NOTE 2	FILL 1.25 NOTE 5	MASS ORDINATE NOTE 6
400'H+20	113	0	0	0	0	0	0	0	0
400'H+50	113	0	126	0	0	126	0	0	126
401'H+00	65	4	165	4	5	291	4	5	286
401'H+50	24	34	82	35	44	373	39	49	324
402'H+00	33	120	53	143	179	426	182	228	198
402'H+50	2	231	32	325	407	458	507	635	-177
403'H+00	2	366	4	553	692	462	1060	1327	-865
403'H+50	0	451	2	756	945	464	1816	2272	-1808
404'H+00	0	244	0	644	805	464	2460	3077	-2613
404'H+28.79	105	0	56	130	163	520	2590	3240	-2720
'H' - LINE COLUMN SUBTOTALS = 520 2590 3240 520 2590 3240 -2720									

EARTHWORK - 'J'-LINE (STH 39)

STATION	AREA (SF)		INCREMENTAL VOLUME (CY)			CUMULATIVE VOLUME (CY)			
	CUT	FILL	CUT NOTE 1	FILL NOTE 2	FILL 1.25 NOTE 5	CUT 1.00 NOTE 1	FILL NOTE 2	FILL 1.25 NOTE 5	MASS ORDINATE NOTE 6
400'J+50	12	41	0	0	0	0	0	0	0
401'J+00	12	41	22	76	95	22	76	95	-73
401'J+50	12	54	22	88	110	44	164	205	-161
402'J+00	12	56	22	102	128	66	266	333	-267
402'J+50	14	39	24	88	110	90	354	443	-353
403'J+00	16	23	28	57	71	118	411	514	-396
403'J+50	17	12	31	32	40	149	443	554	-405
404'J+00	21	7	35	18	22	184	461	576	-392
404'J+50	22	7	40	13	16	224	474	592	-368
405'J+00	18	0	37	6	8	261	480	600	-339
405'J+50	21	0	36	0	0	297	480	600	-303
406'J+00	24	0	42	0	0	339	480	600	-261
406'J+50	82	0	98	0	0	437	480	600	-163
407'J+00	77	0	147	0	0	584	480	600	-16
407'J+47.45	78	0	136	0	0	720	480	600	120
'J' - LINE COLUMN SUBTOTALS = 720 480 600 720 480 600 120									

NOTES:

1 - CUT	CUT INCLUDES SALVAGED/UNUSABLE MATERIAL
2 - FILL	DOES NOT INCLUDE UNUSABLE PAVEMENT EXCAVATION VOLUMES
3 - EXPANDED ROCK	EXPANDED ROCK THAT CAN BE USED AS FILL
4 - REDUCED MARSH IN FILL	REDUCED MARSH THAT CAN BE USED AS FILL
5 - FILL (25%)	FILL 25% = (FILL - EXPANDED ROCK (1.1) - REDUCED MARSH (0.6))*1.25
6 - MASS ORDINATE	CUT - FILL (25%)

NOTE: EARTHWORK QUANTITIES FOR THE MAINLINE INCLUDE THE QUANTITIES FOR ALIGNMENTS 'K', 'L', 'M', 'O', 'P', 'Q', AND 'R'-LINES.

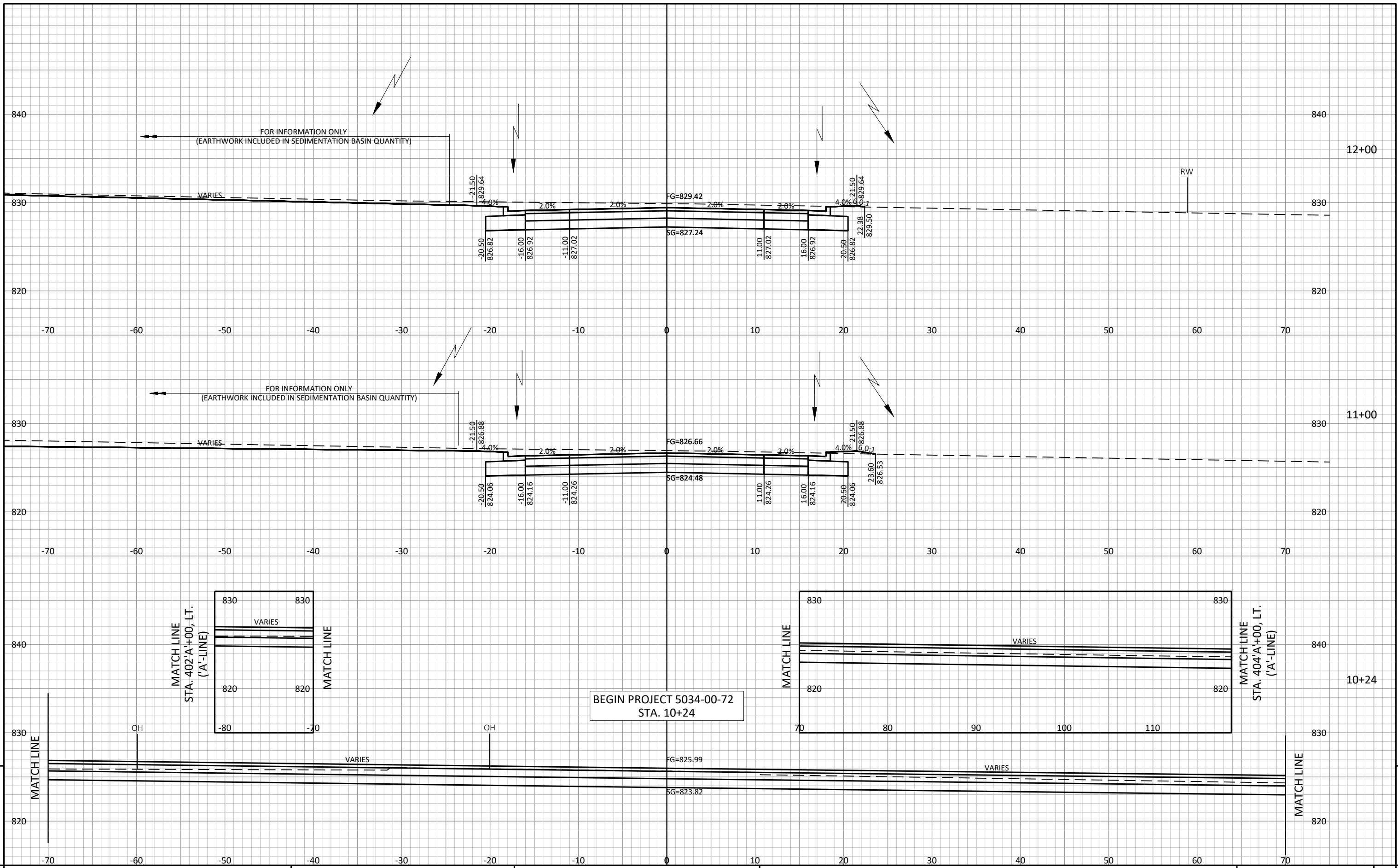
EARTHWORK SUMMARY

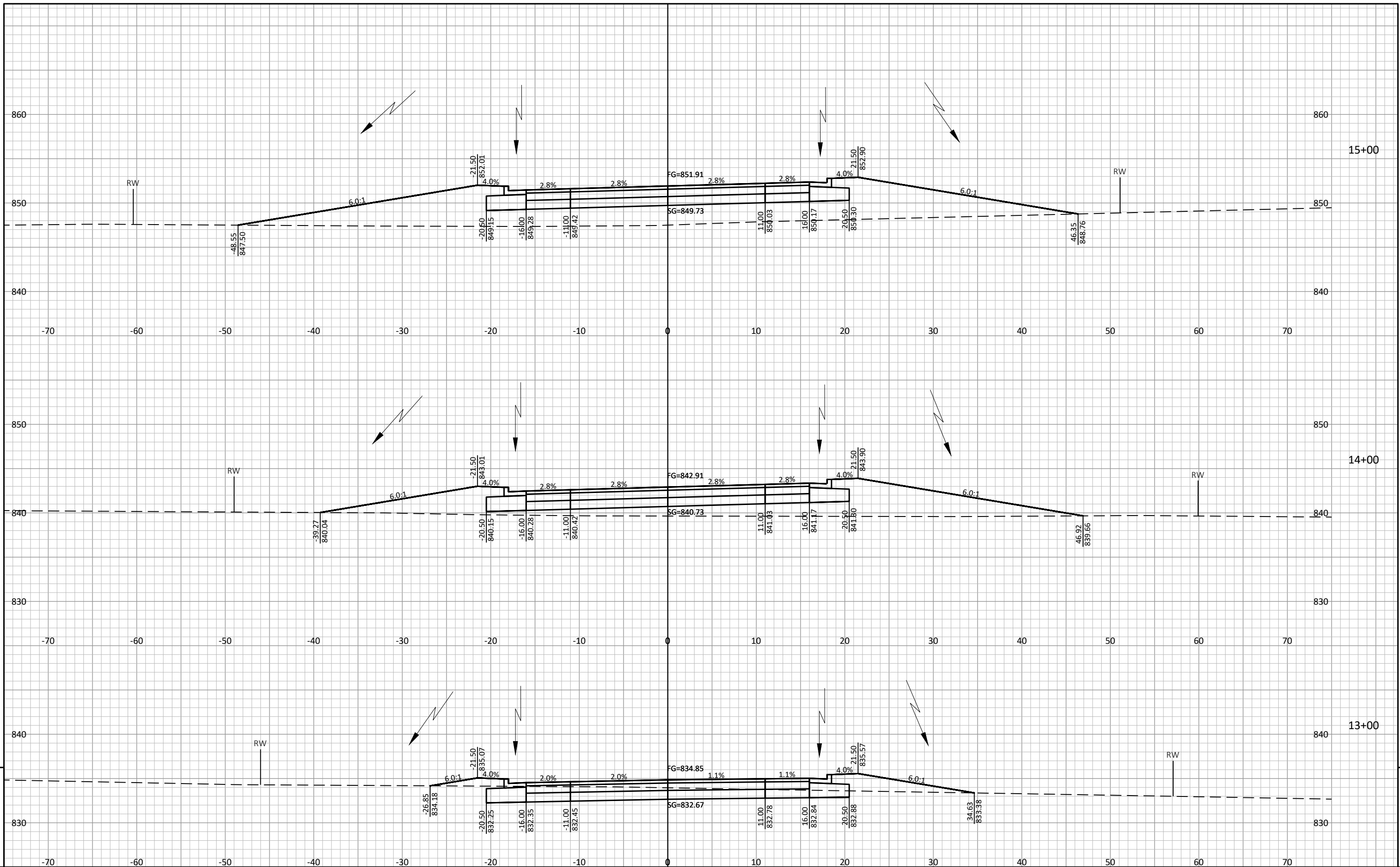
STATION - STATION	LOCATION	(1) 205.0100 EXCAVATION COMMON			UNEXPANDED FILL			(2) 205.0200 EXCAVATION ROCK	(3) EXPANDED ROCK (CY)	(4) 205.0400 EXCAVATION MARSH	(5) REDUCED MARSH (CY)	(6) EXPANDED FILL FACTOR 1.25			(7) MASS ORDINATE +/-			208.0100 BORROW		
		(CAT 010) (CY)	(CAT 020) (CY)	(CAT 030) (CY)	(CAT 010) (CY)	(CAT 020) (CY)	(CAT 030) (CY)	(CAT 010) (CY)	FACTOR 1.1	(CAT 010) (CY)	FACTOR 0.6	(CAT 010) (CY)	(CAT 020) (CY)	(CAT 030) (CY)	(CAT 010) (CY)	(CAT 020) (CY)	(CAT 030) (CY)	(CAT 010) (CY)	(CAT 020) (CY)	(CAT 030) (CY)
10+24 - 226+00	MAINLINE	36130	0	0	52380	0	0	1600	1760	2100	1260	61700	0	0	-25570	0	0	25570	0	0
226+00 - 265+24.10	MAINLINE	0	3670	0	0	8500	0	-	-	-	-	0	10625	0	0	-6955	0	0	6955	0
399'A'+55 - 406'A'+98.30	'A'-LINE (STH 78)	1035	0	0	60	0	0	-	-	-	-	75	0	0	960	0	0	-960	0	0
399'B'+62.87 - 402'B'+03.52	'B'-LINE (MOUND STREET)	1070	0	0	65	0	0	-	-	-	-	80	0	0	990	0	0	-990	0	0
400'C'+00 - 401'C'+68.23	'C'-LINE (P.E.)	1070	0	0	20	0	0	-	-	-	-	25	0	0	1045	0	0	-1045	0	0
400'D'+00 - 402'D'+87.03	'D'-LINE (SHARED USE ACCESS)	910	0	0	200	0	0	-	-	-	-	250	0	0	660	0	0	-660	0	0
401'E'+88 - 403'E'+24.08	'E'-LINE (HORSESHOE BEND ROAD)	520	0	0	15	0	0	-	-	-	-	20	0	0	500	0	0	-500	0	0
400'F'+16 - 402'F'+20	'F'-LINE (WEST MOSCOW ROAD)	275	0	0	90	0	0	-	-	-	-	115	0	0	160	0	0	-160	0	0
400'G'+15 - 406'G'+64.33	'G'-LINE (DRIVEWAY REALIGNMENT)	1230	0	0	65	0	0	-	-	-	-	80	0	0	1150	0	0	-1150	0	0
400'H'+20 - 404'H'+28.79	'H'-LINE (MCKENNA ROAD)	520	0	0	2590	0	0	-	-	-	-	3240	0	0	-2720	0	0	2720	0	0
402'I'+50 - 409'I'+24.87	'I'-LINE (EAST PECATONICA ROAD)	0	350	0	0	16980	0	-	-	-	-	0	21225	0	0	-20875	0	0	20875	0
400'J'+50 - 407'J'+47.45	'J'-LINE (STH 39)	0	720	0	0	480	0	-	-	-	-	0	600	0	0	120	0	0	-120	0
1'N'+00 - 2'N'+20	N'-LINE (CATTLE WALKWAY)	0	0	970	0	0	0	-	-	-	-	0	0	0	0	0	970	0	0	-970
-	P.E./C.E./F.E.	3900	800	0	1950	400	0	-	-	-	-	2440	500	0	1460	300	0	-1460	-300	0
-	STORMWATER POND	2020	0	0	0	0	0	-	-	-	-	0	0	0	2020	0	0	-2020	0	0
CATEGORY TOTALS =		48680	5540	970	57435	26360	0	1600	1760	2100	1260	68025	32950	0	-19345	-27410	970	19345	27410	-970
PROJECT TOTALS =			55190			83795		1600	1760	2100	1260		100975			-45785			45785	

NOTES:

1 - CUT	CUT INCLUDES SALVAGED/UNUSABLE MATERIAL
2 - FILL	DOES NOT INCLUDE UNUSABLE PAVEMENT EXCAVATION VOLUMES
3 - EXPANDED ROCK	EXPANDED ROCK THAT CAN BE USED AS FILL
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6 - MASS ORDINATE	CUT - FILL (25%)

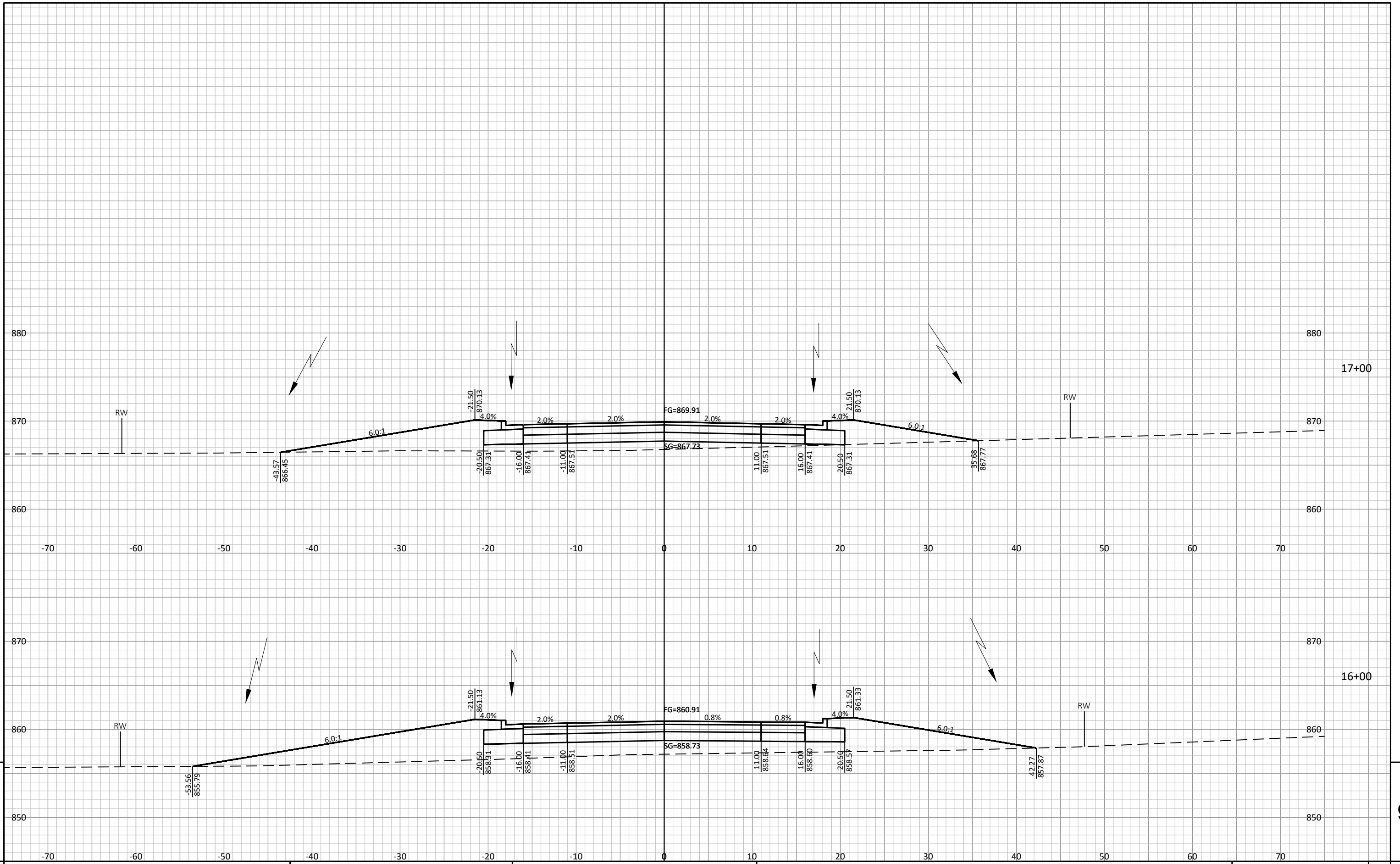
NOTE: EARTHWORK QUANTITIES FOR THE MAINLINE INCLUDE THE QUANTITIES FOR ALIGNMENTS 'K', 'L', 'M', 'O', 'P', 'Q', AND 'R'-LINES.





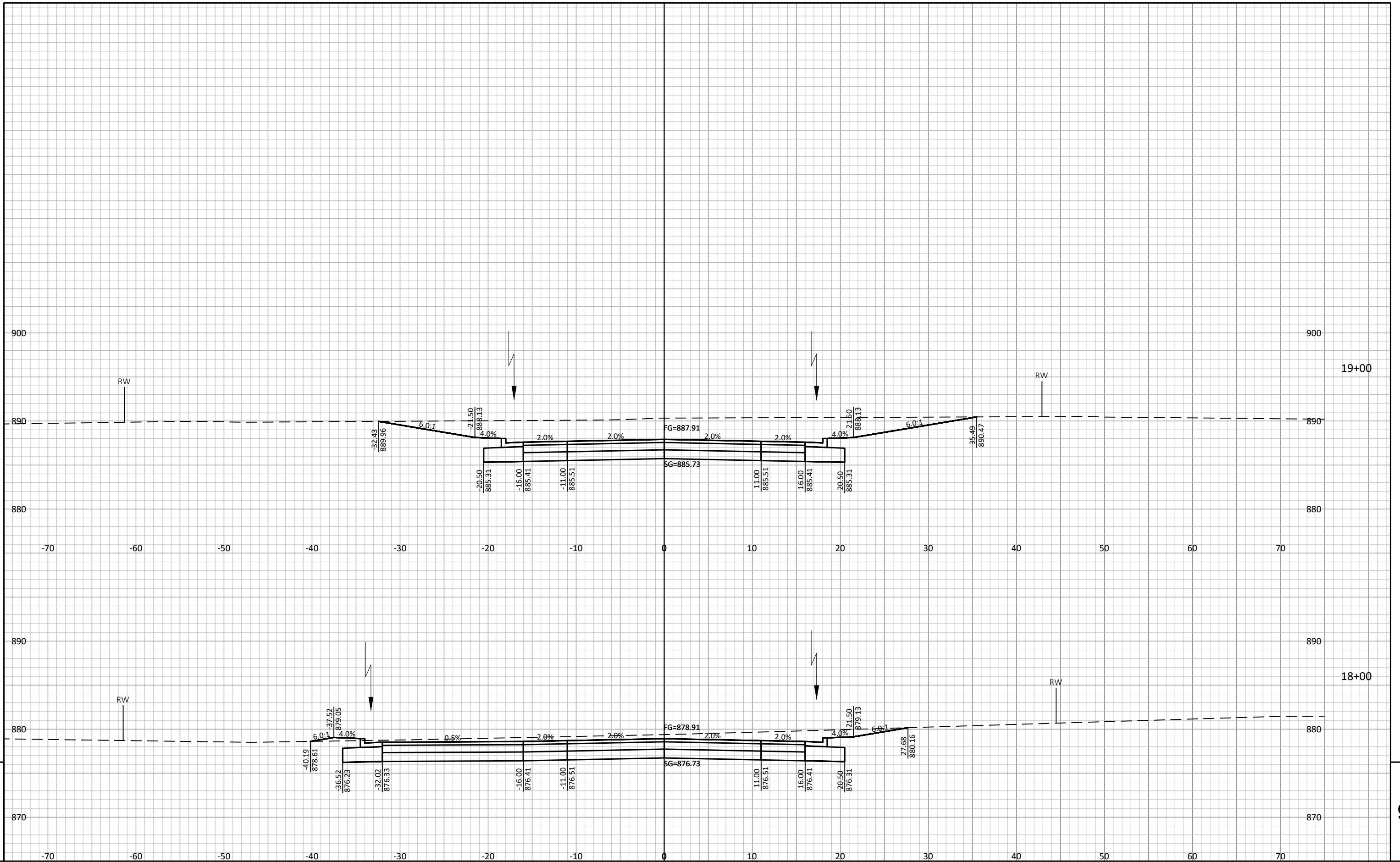
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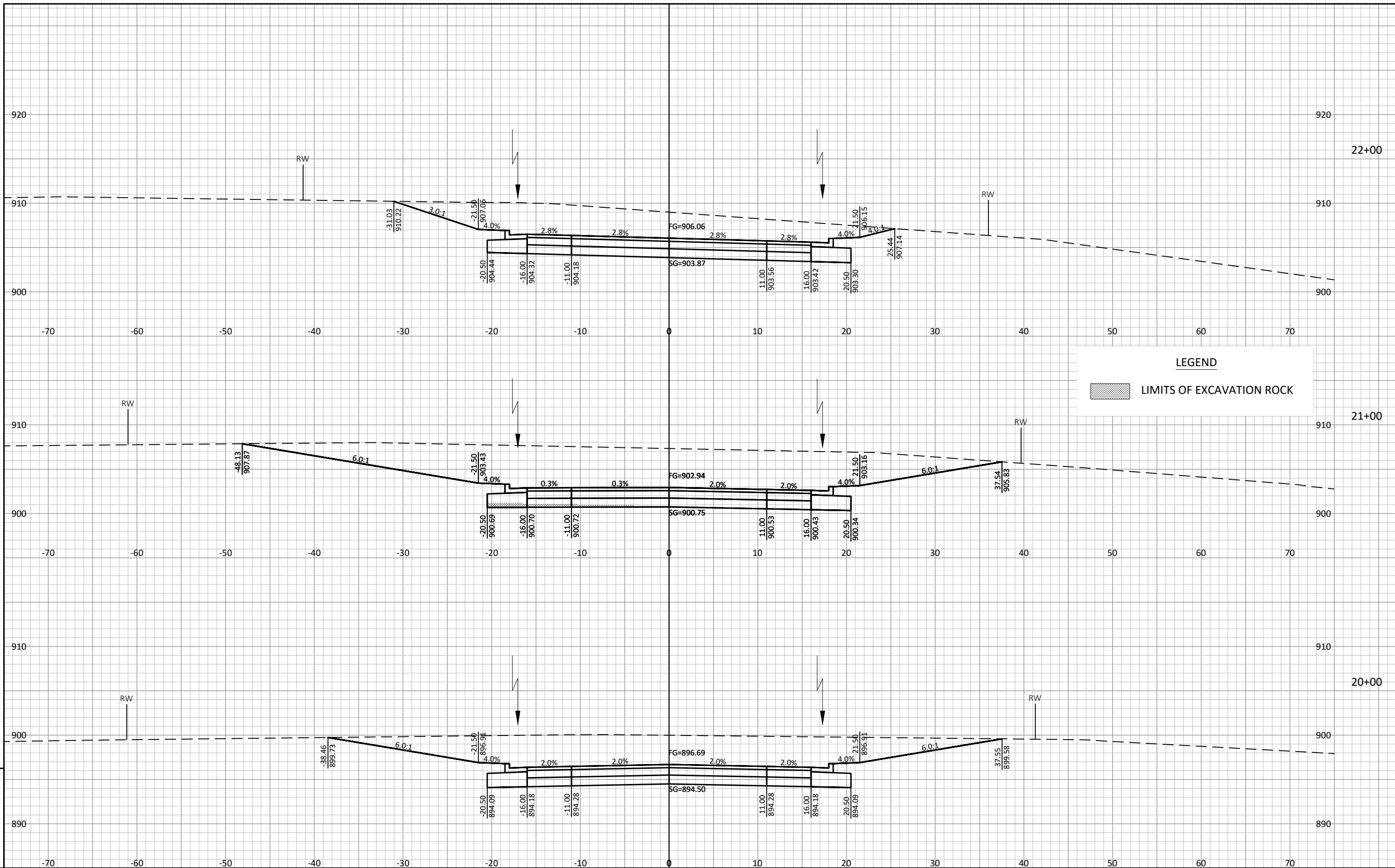
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


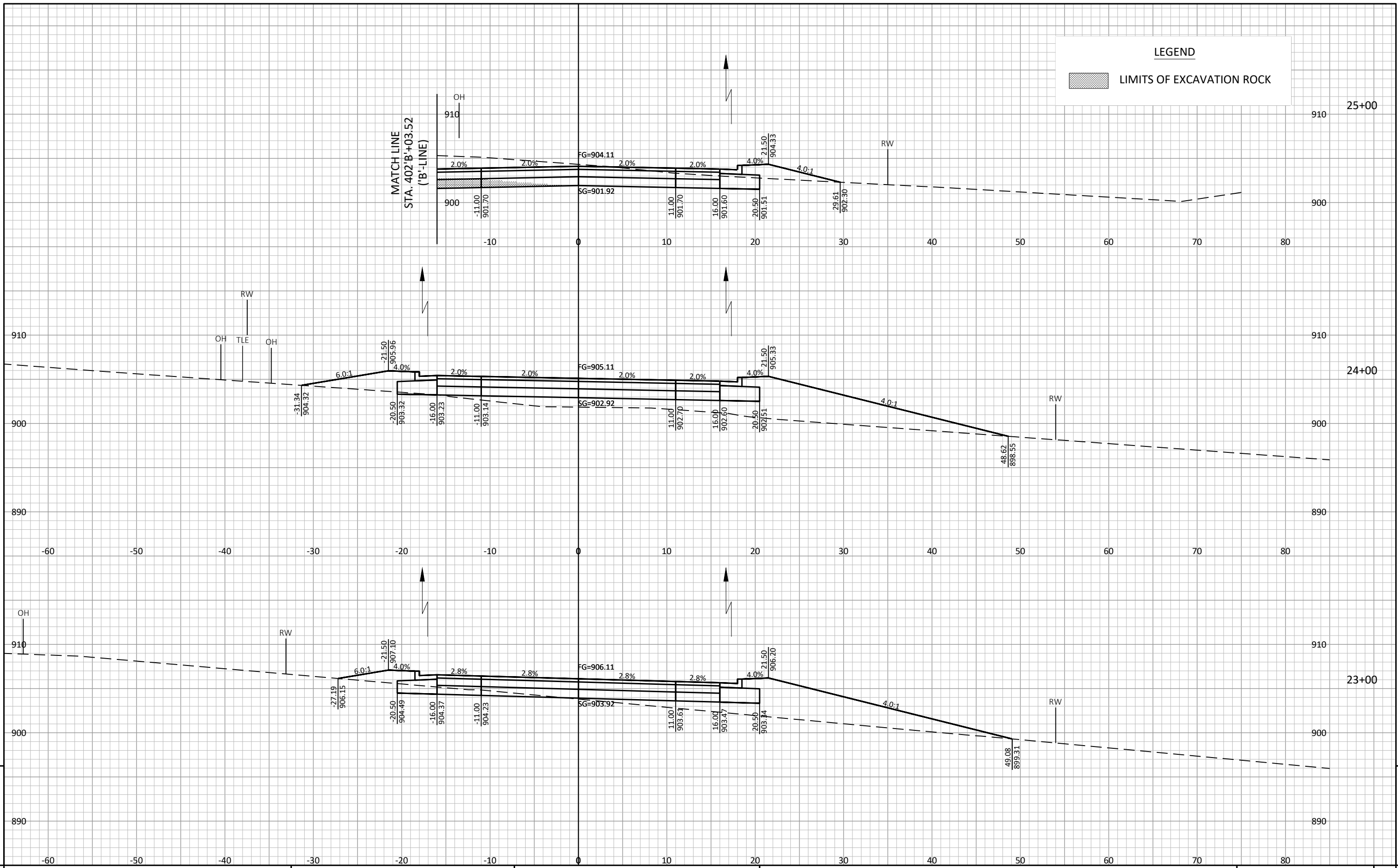
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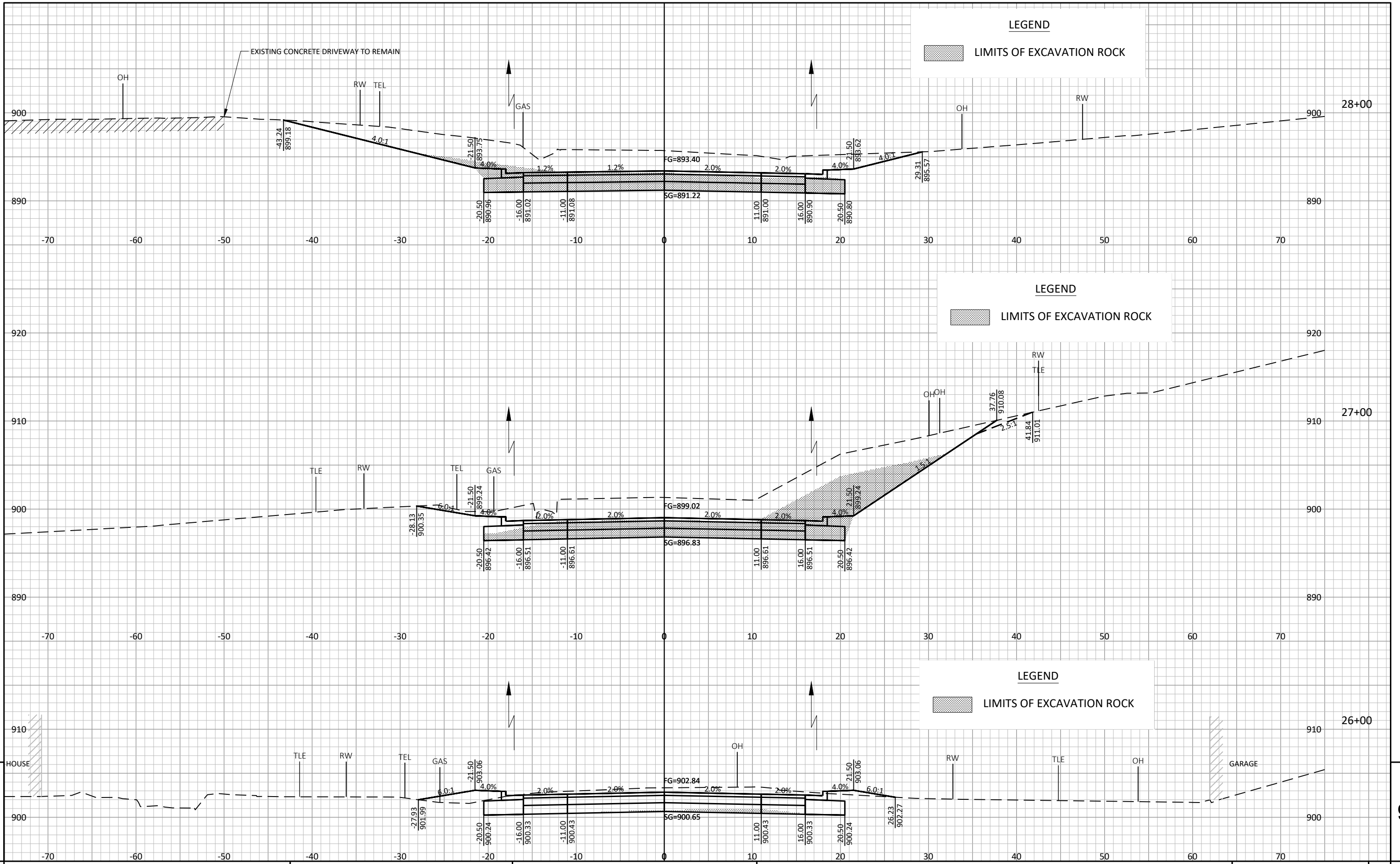
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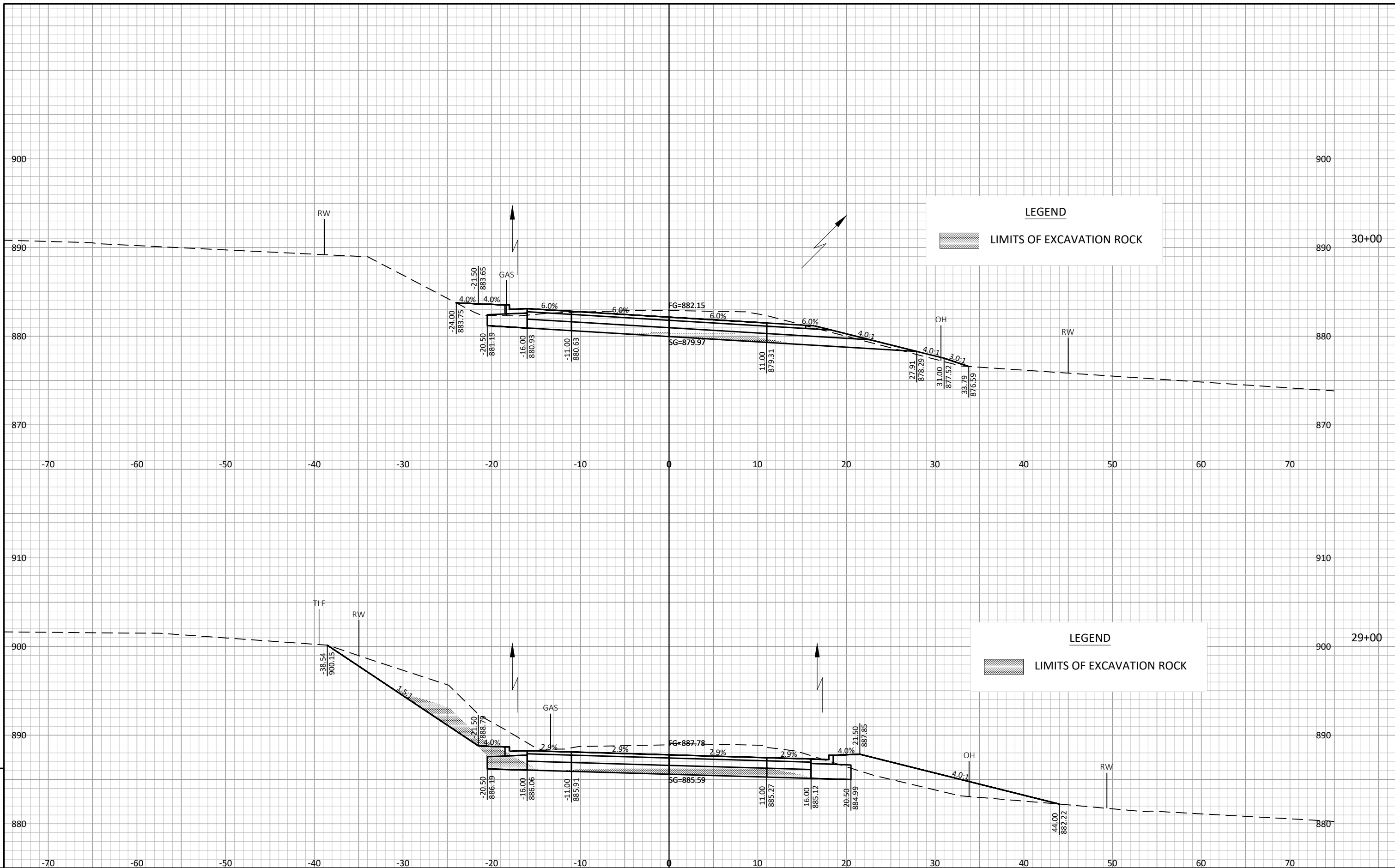
 LIMITS OF EXCAVATION ROCK

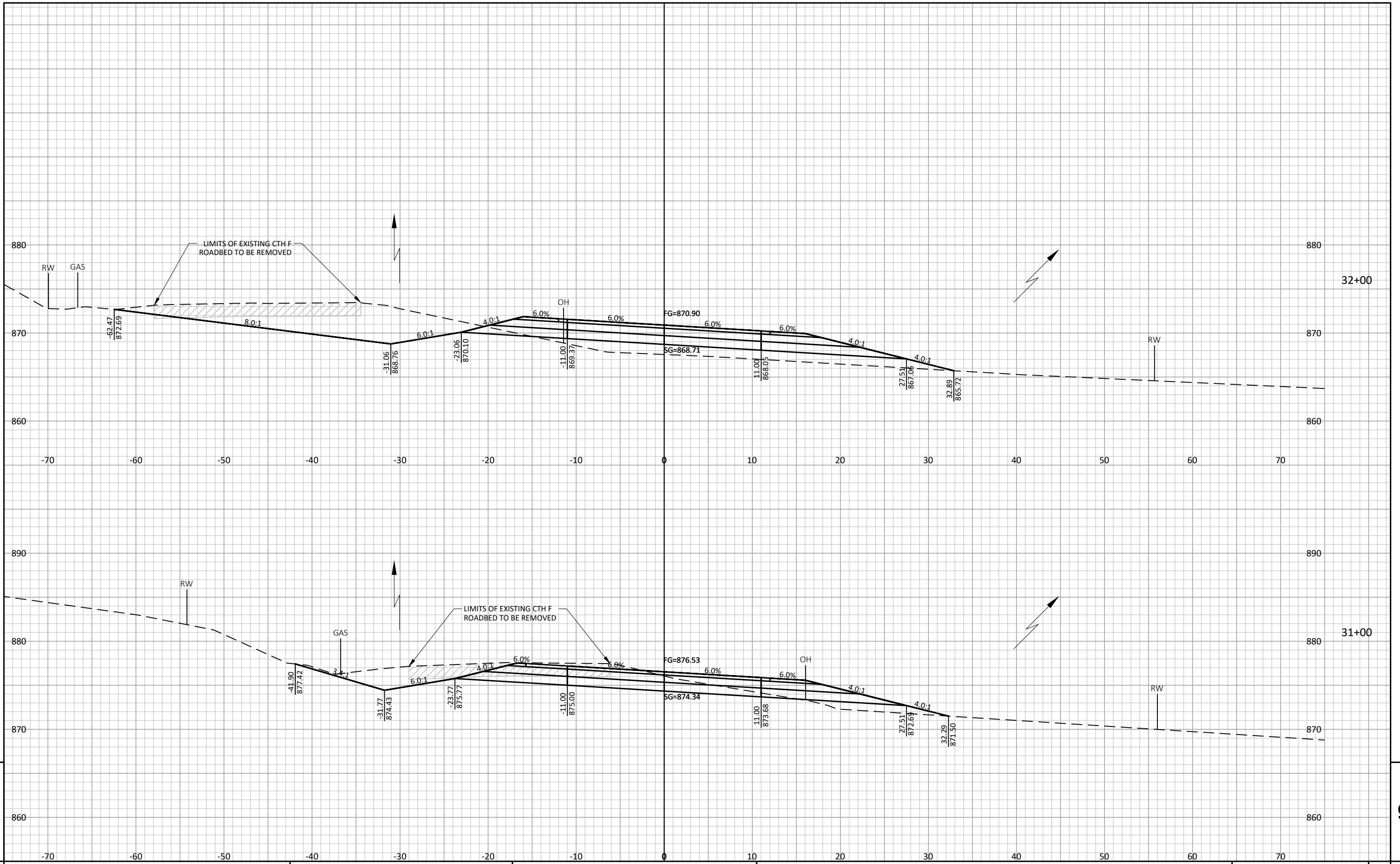


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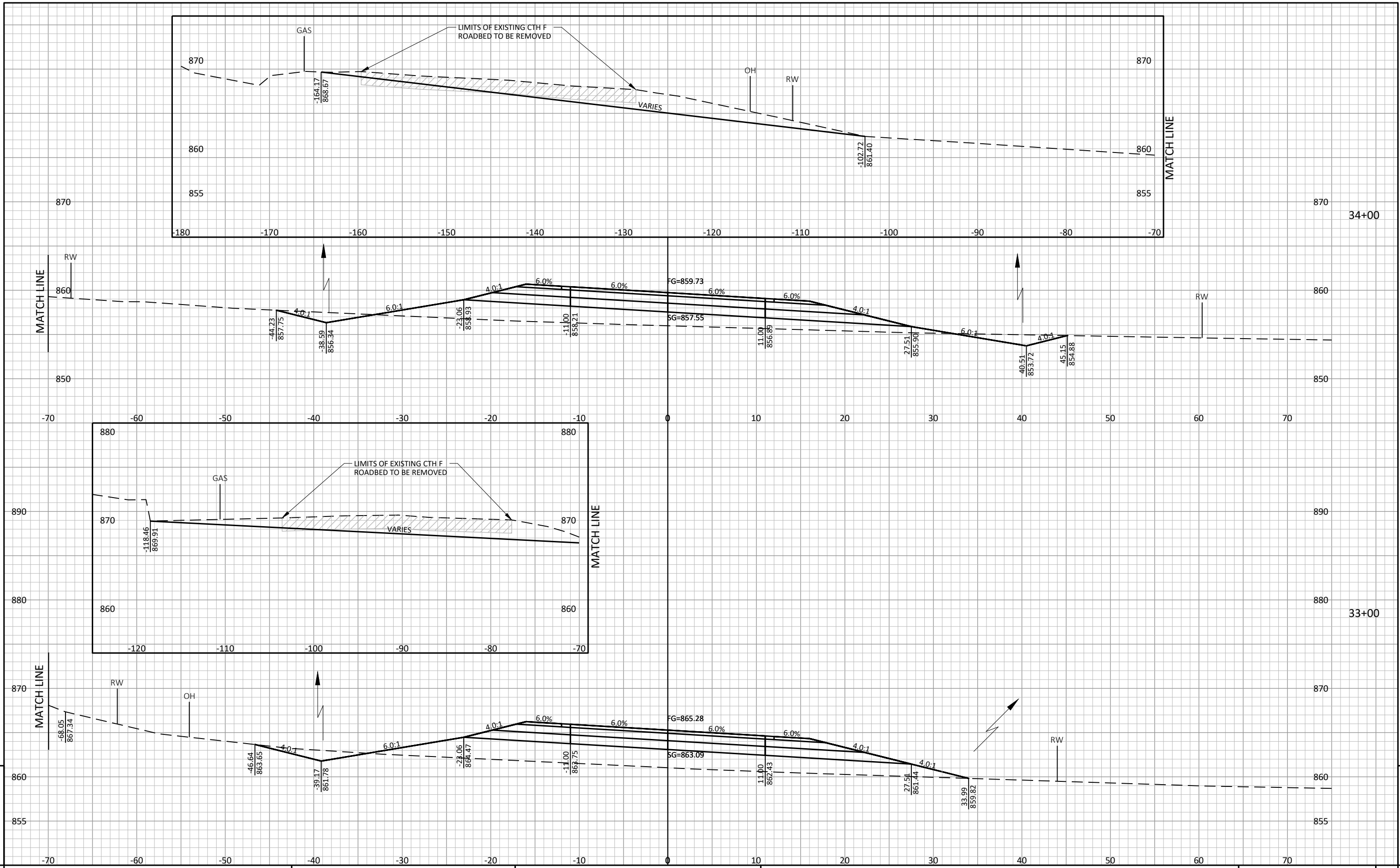






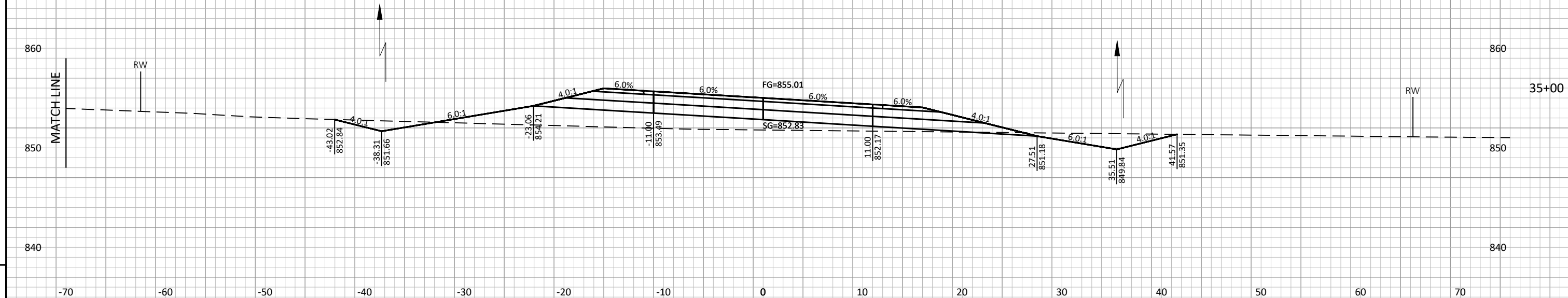
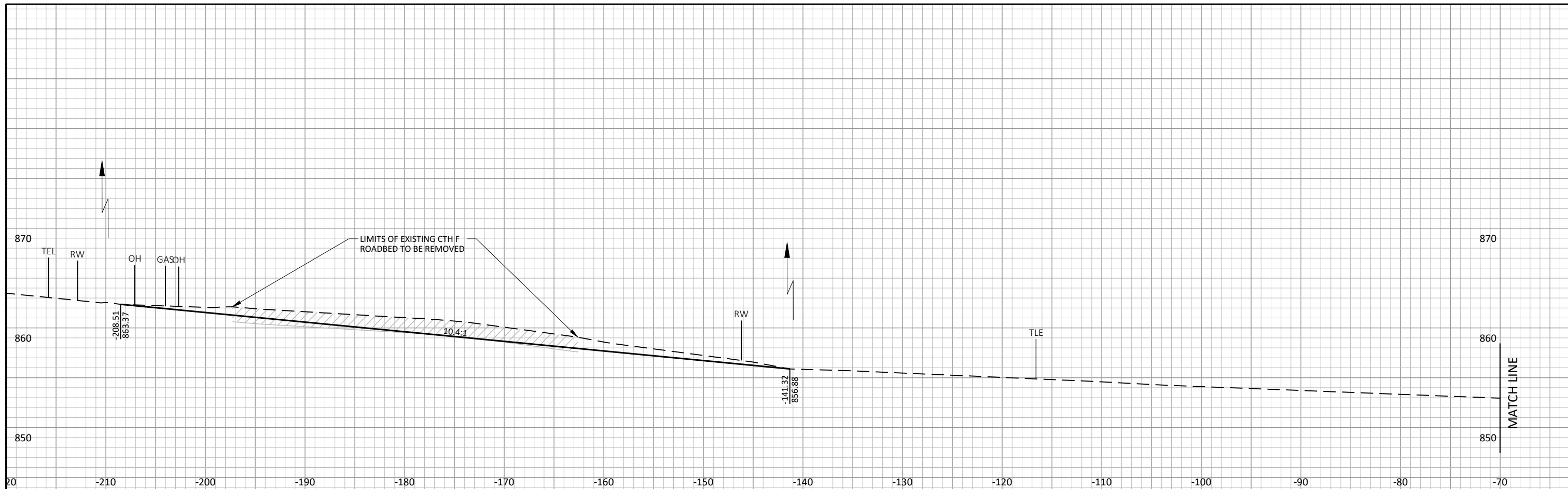
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PROJECT NO: 5034-00-72

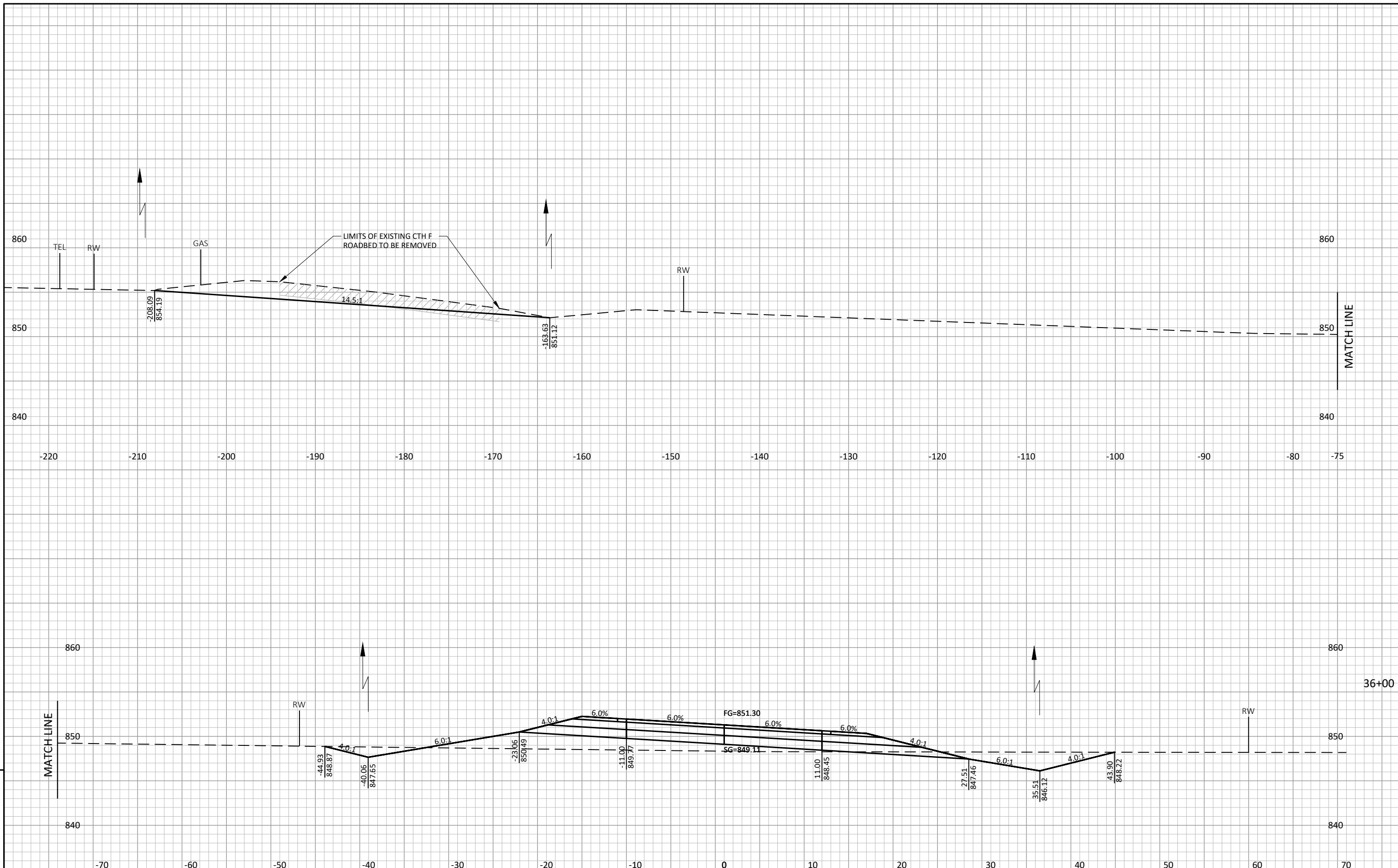
HWY: CTH F

COUNTY: IOWA

CROSS SECTIONS: MAINLINE

SHEET

E



PROJECT NO: 5034-00-72

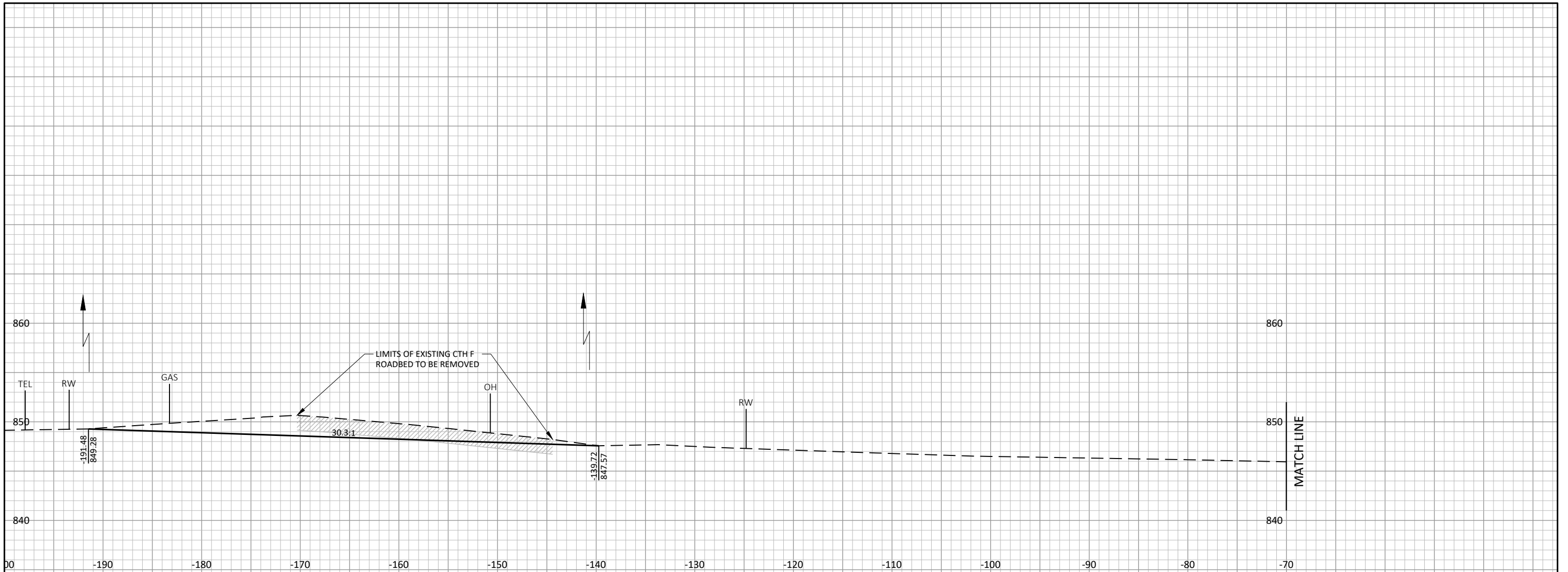
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COUNTY: IOWA

CROSS SECTIONS: MAINLINE

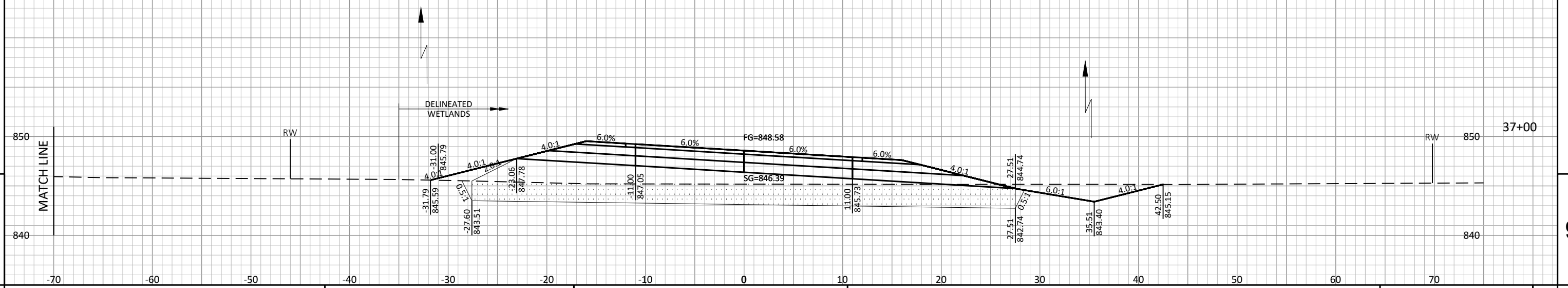
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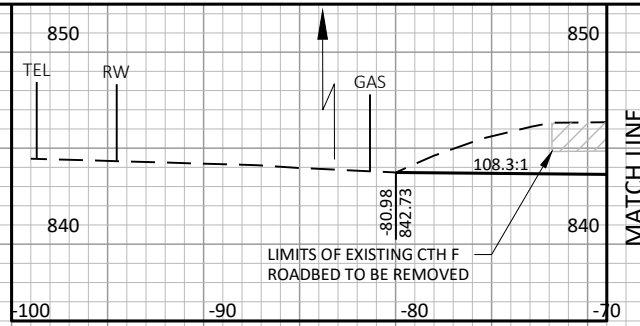
LEGEND

LIMITS OF EXCAVATION MARSH



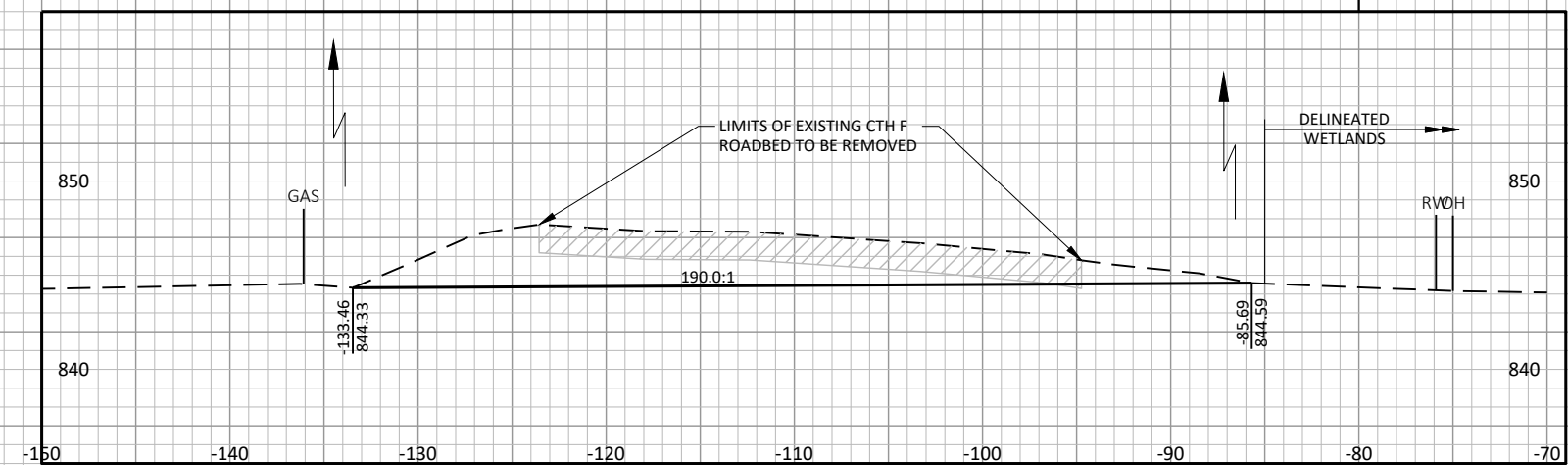
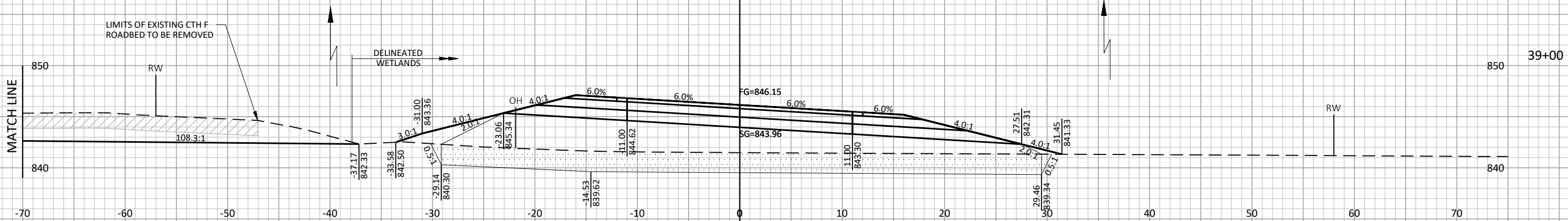
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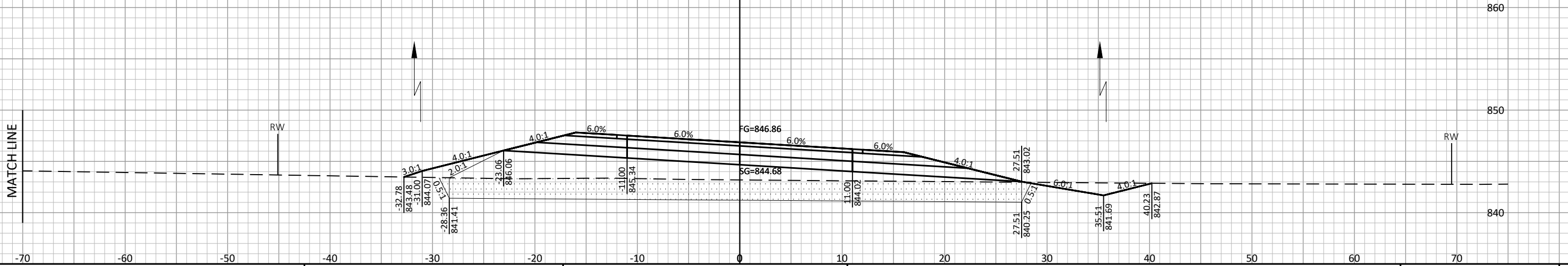
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LIMITS OF EXCAVATION MARSH



LEGEND

LIMITS OF EXCAVATION MARSH



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PROJECT NO: 5034-00-72

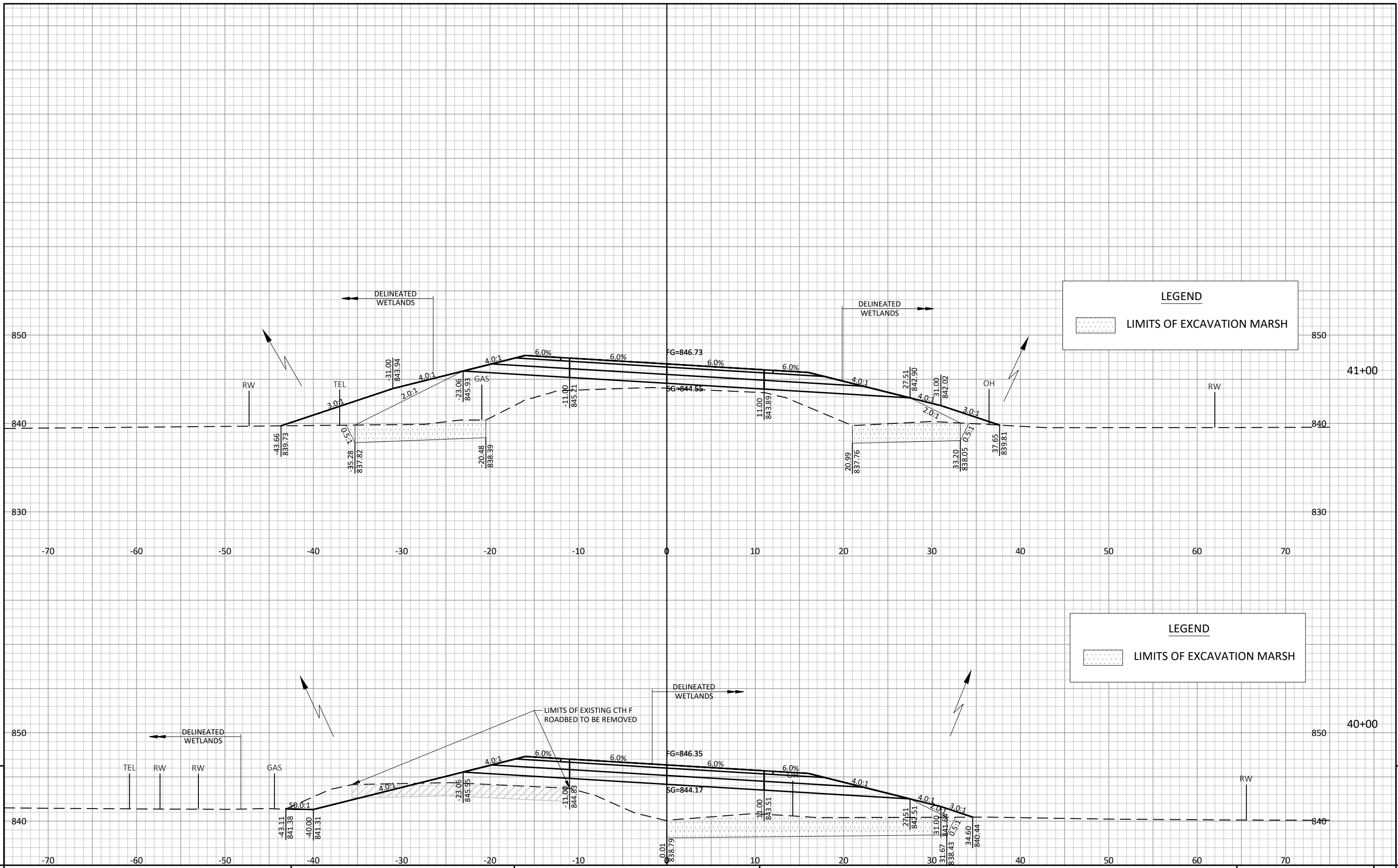
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COUNTY: IOWA


CROSS SECTIONS: MAINLINE

SHEET


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LEGEND

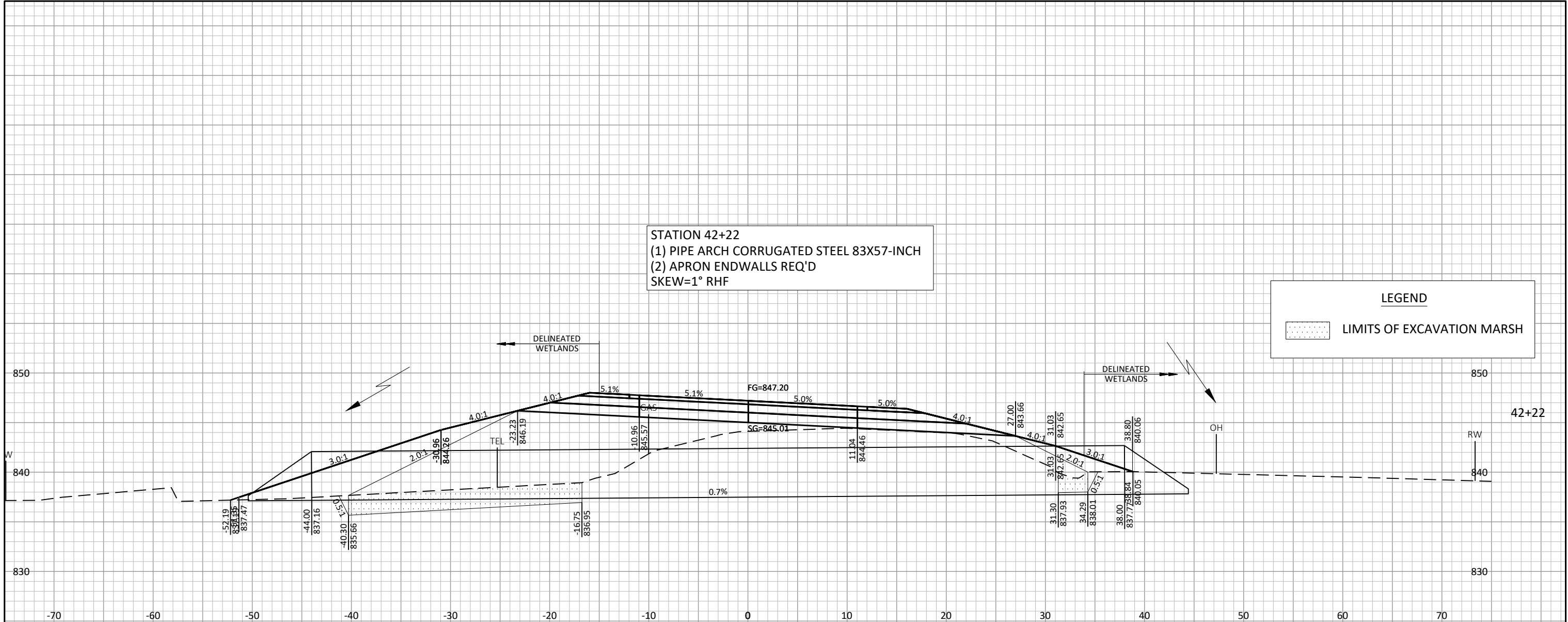
 LIMITS OF EXCAVATION MARSH

LEGEND

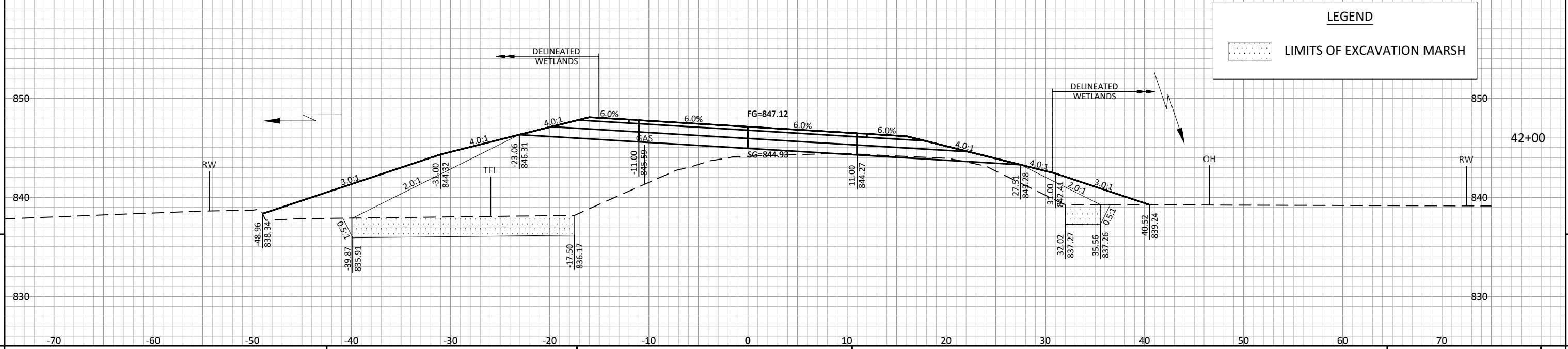
 LIMITS OF EXCAVATION MARSH

STATION 42+22
 (1) PIPE ARCH CORRUGATED STEEL 83X57-INCH
 (2) APRON ENDWALLS REQ'D
 SKEW=1° RHF

LEGEND
 [Dotted Box] LIMITS OF EXCAVATION MARSH

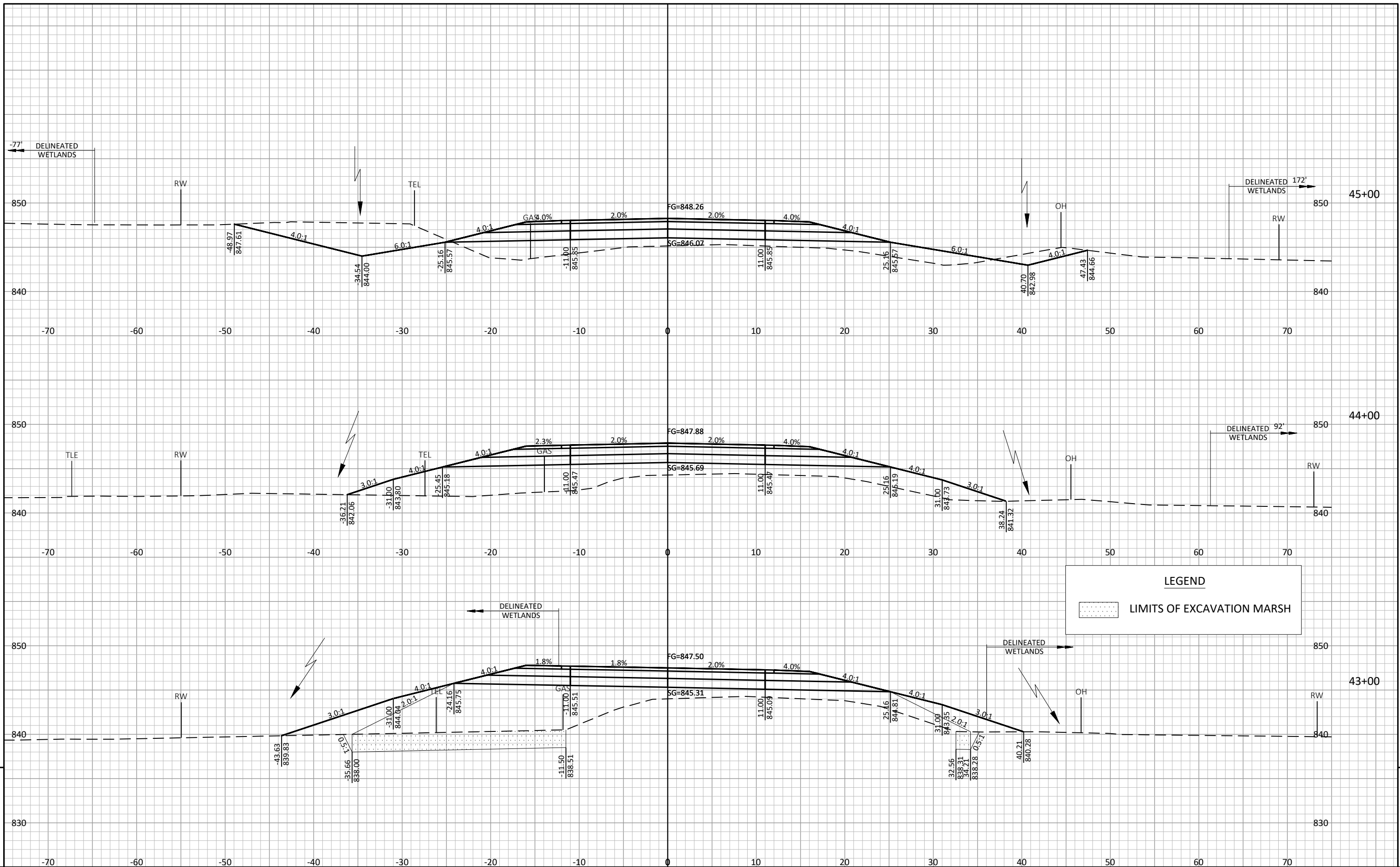


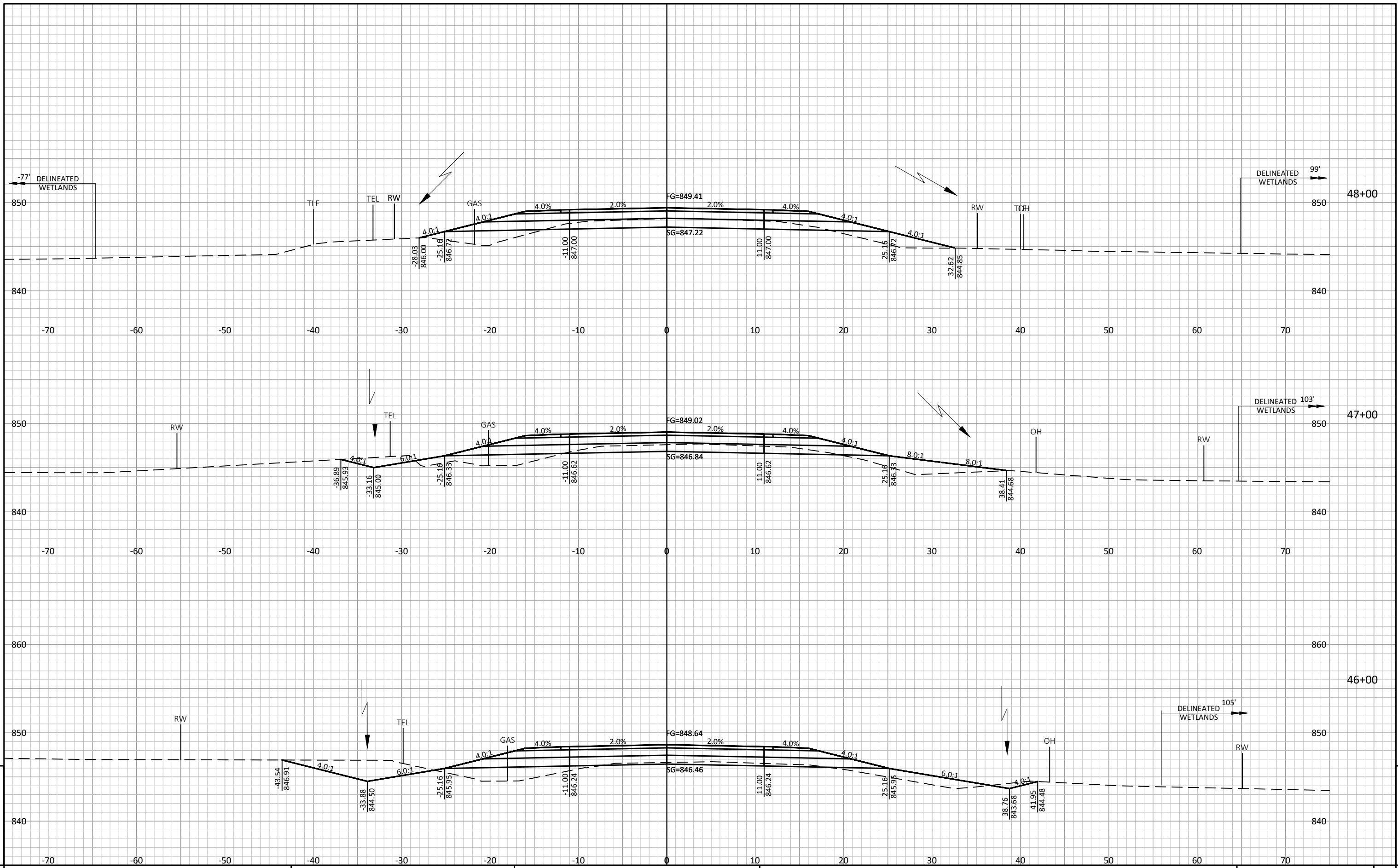
LEGEND
 [Dotted Box] LIMITS OF EXCAVATION MARSH



9

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PROJECT NO: 5034-00-72

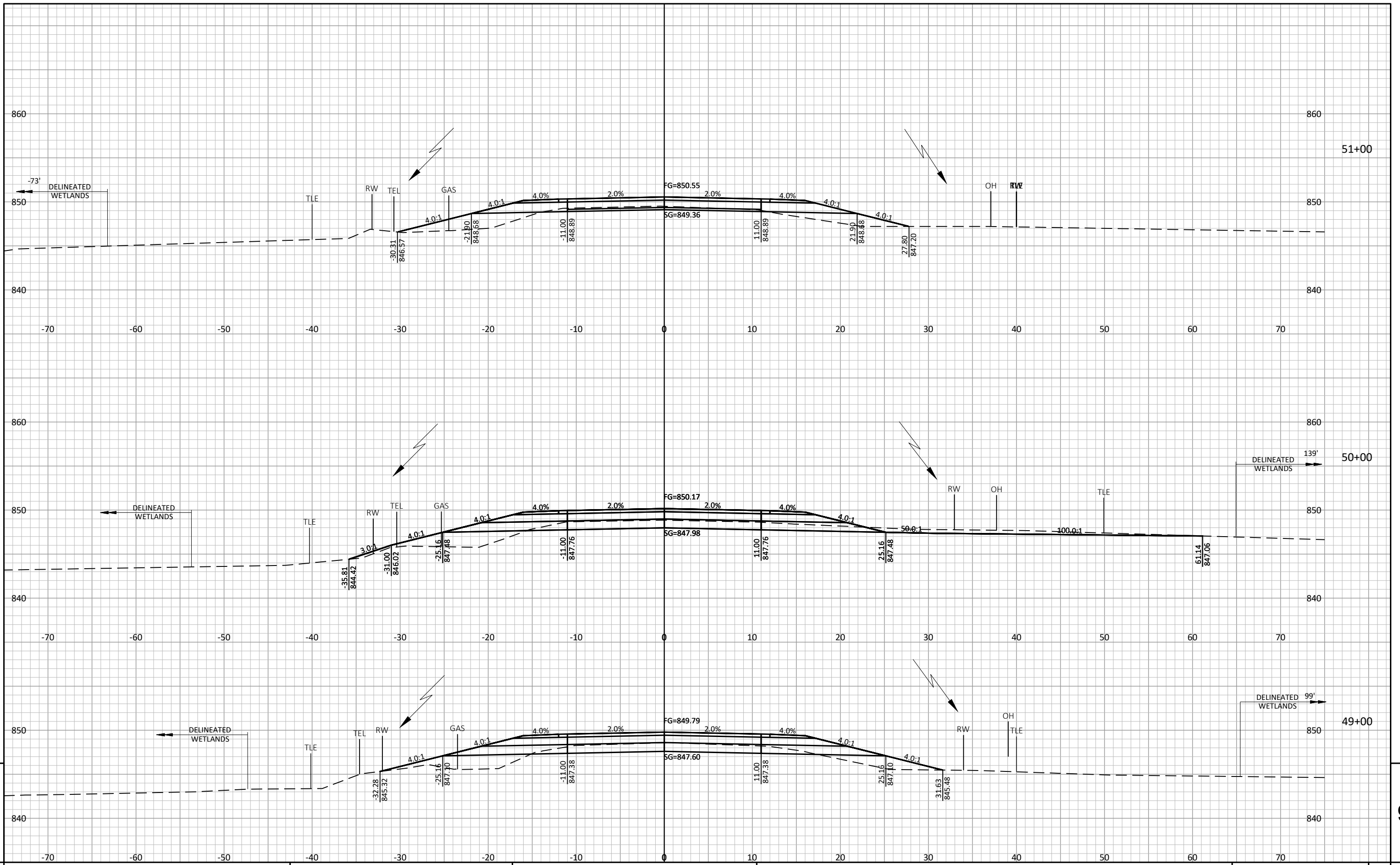
HWY: CTH F

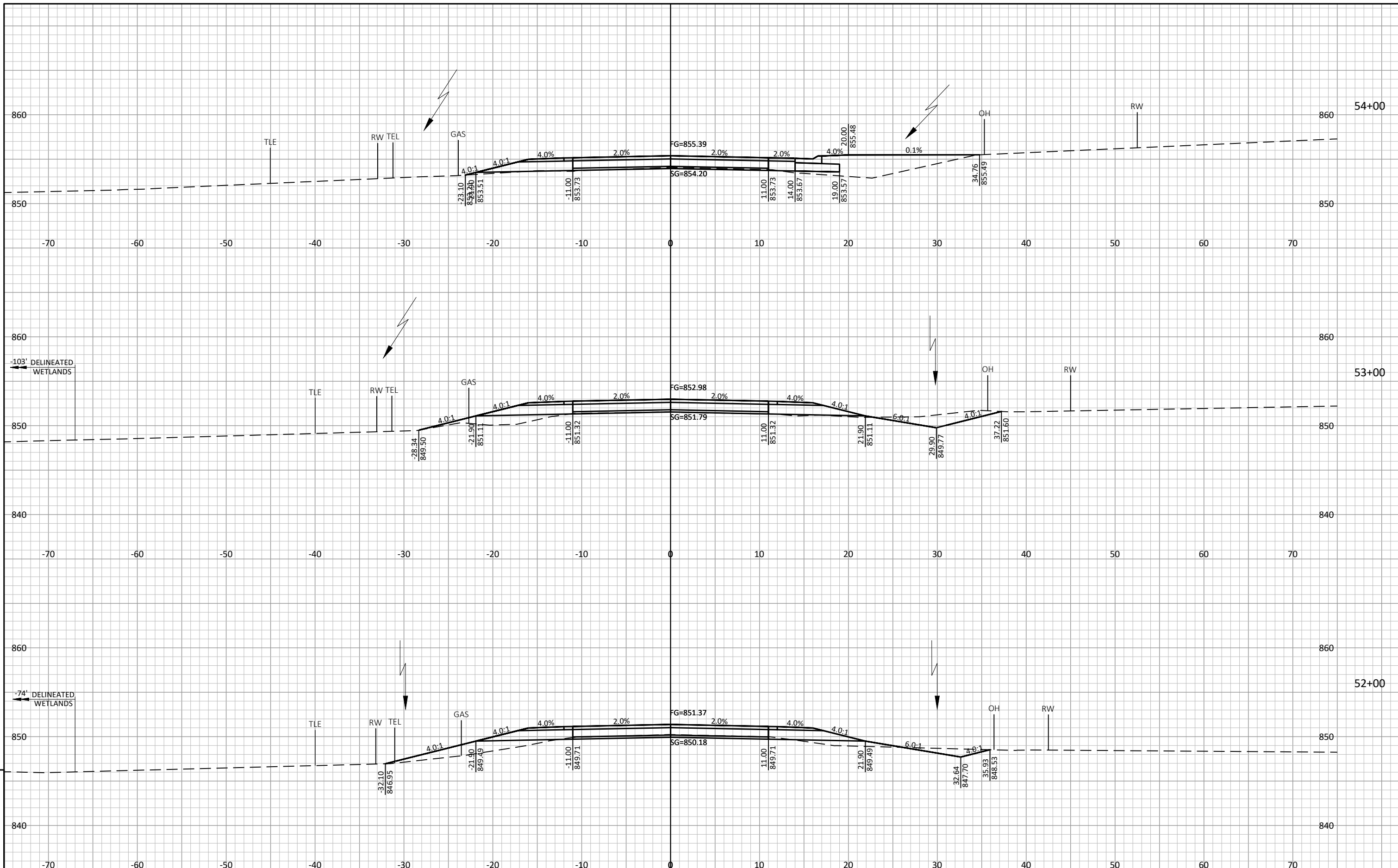
COUNTY: IOWA

CROSS SECTIONS: MAINLINE

SHEET

E





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PROJECT NO: 5034-00-72

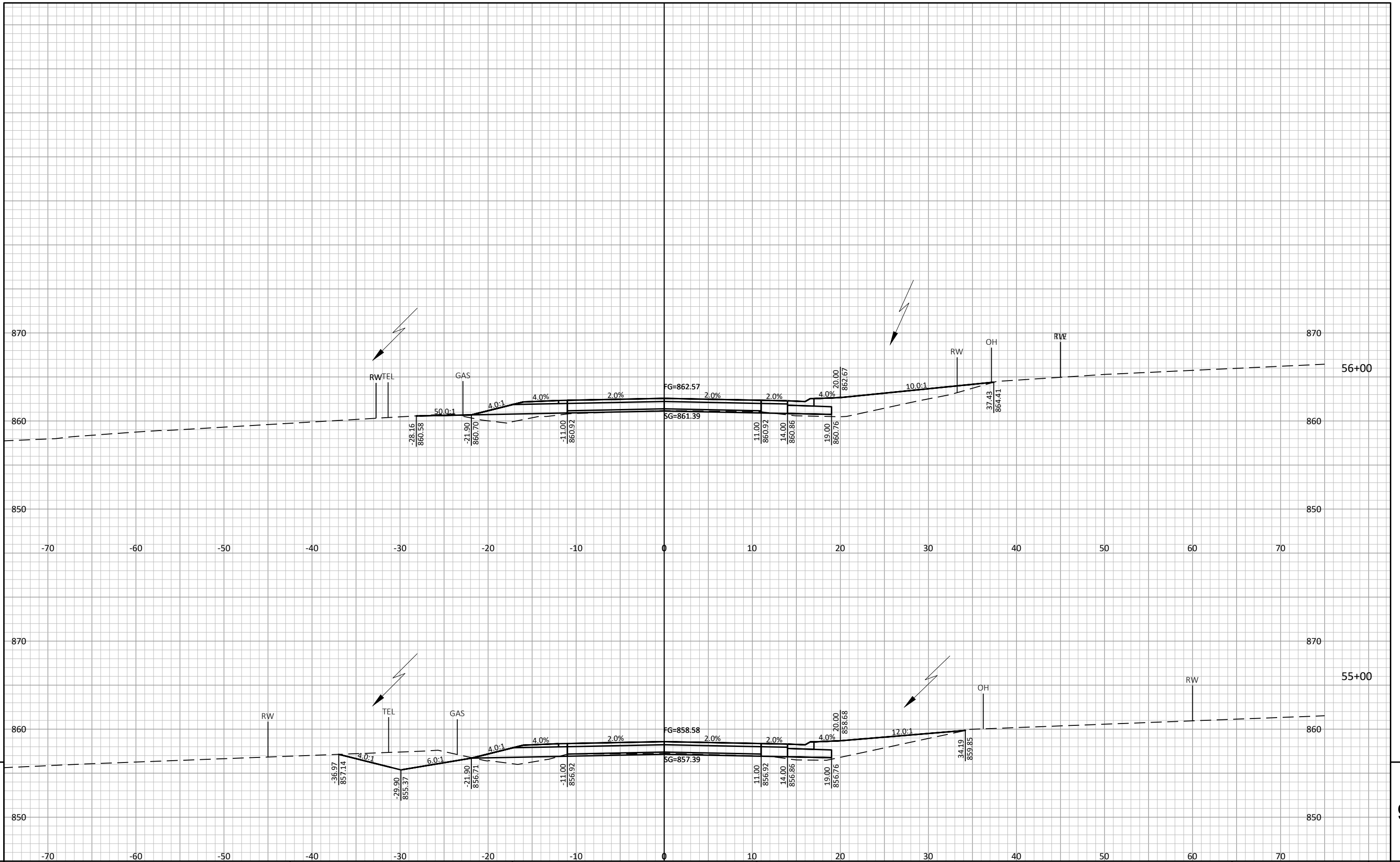
HWY: CTH F

COUNTY: IOWA

CROSS SECTIONS: MAINLINE

SHEET

E



PROJECT NO: 5034-00-72

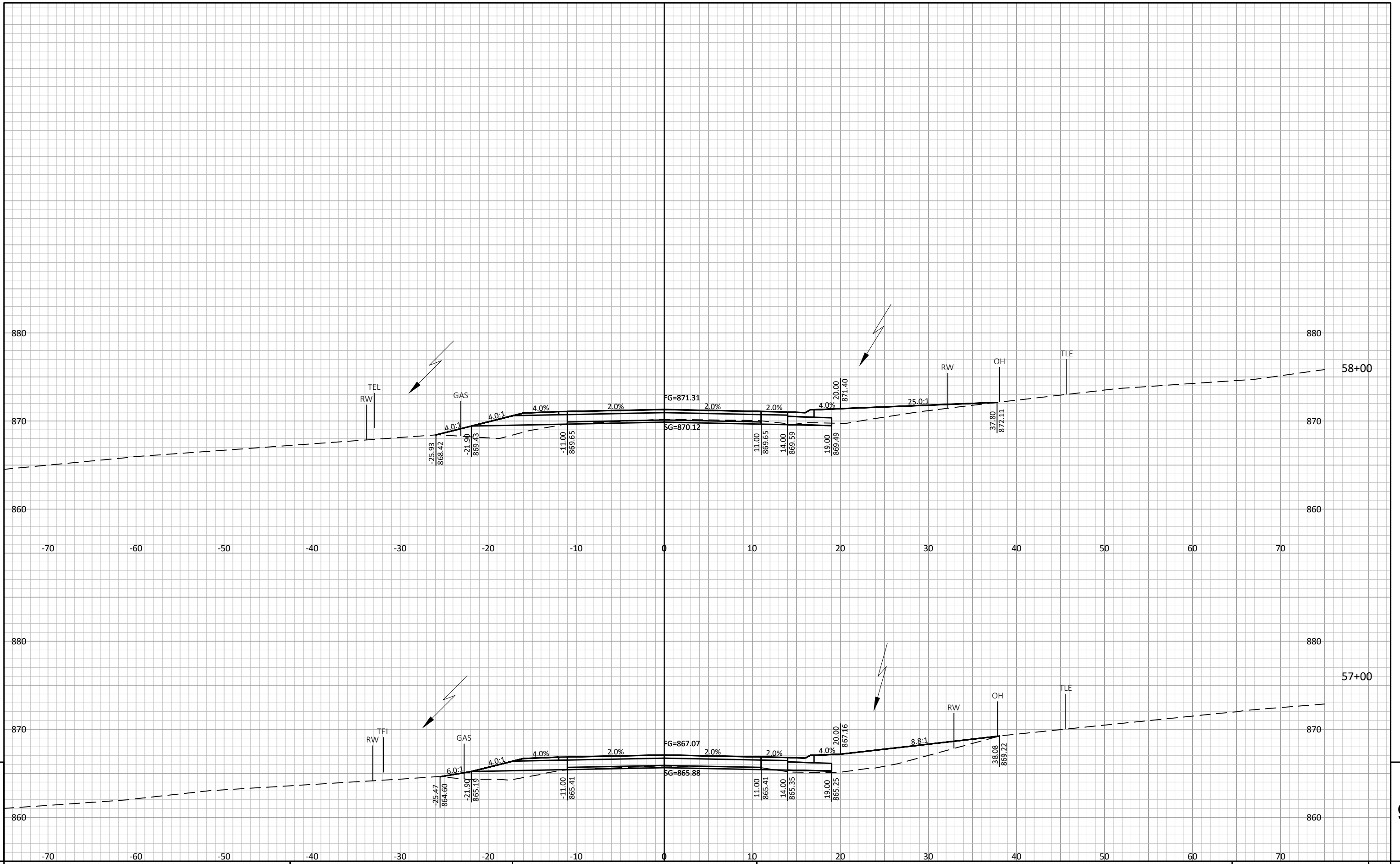
HWY: CTH F

COUNTY: IOWA

CROSS SECTIONS: MAINLINE

SHEET

E



PROJECT NO: 5034-00-72

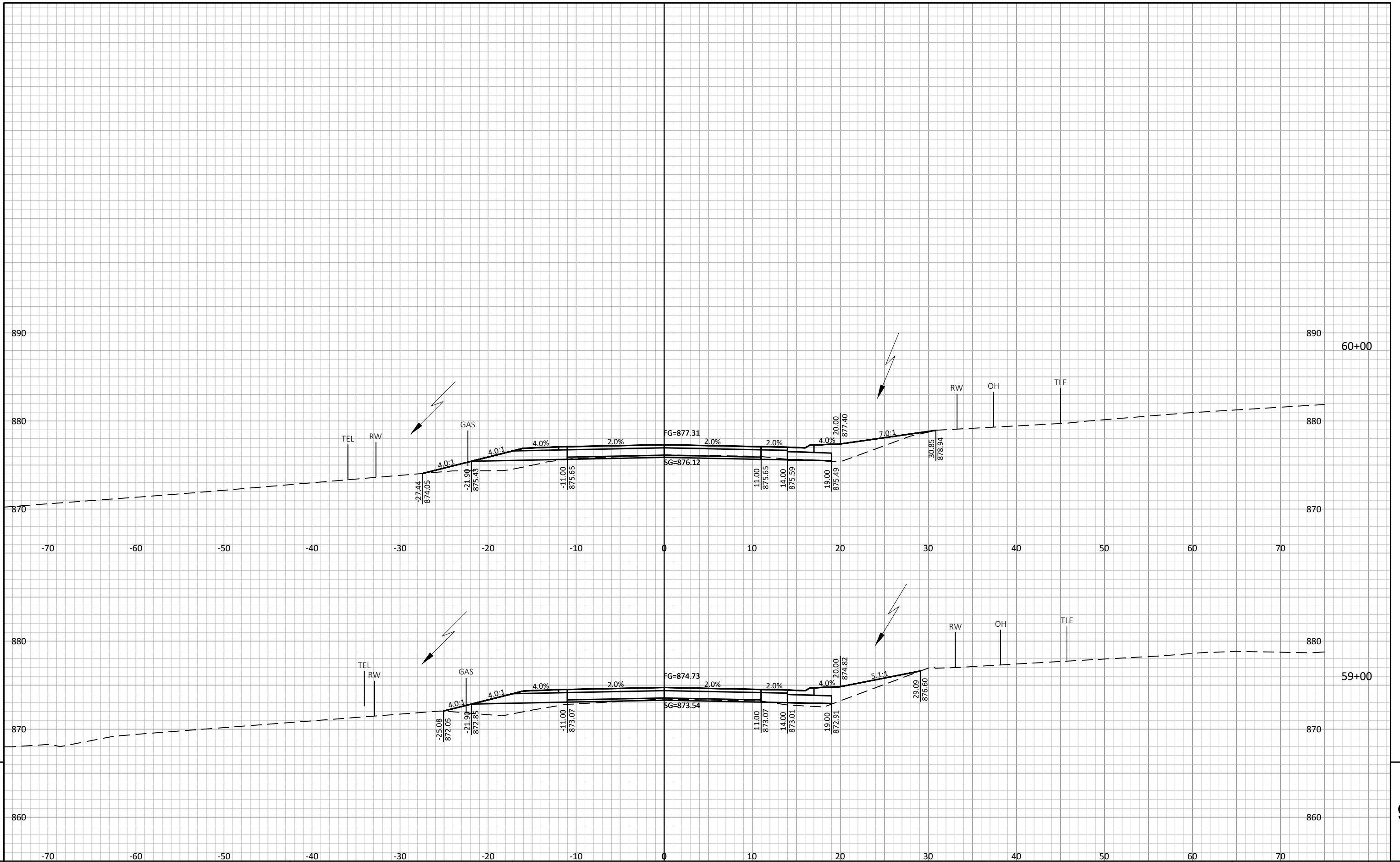
HWY: CTH F

COUNTY: IOWA

CROSS SECTIONS: MAINLINE

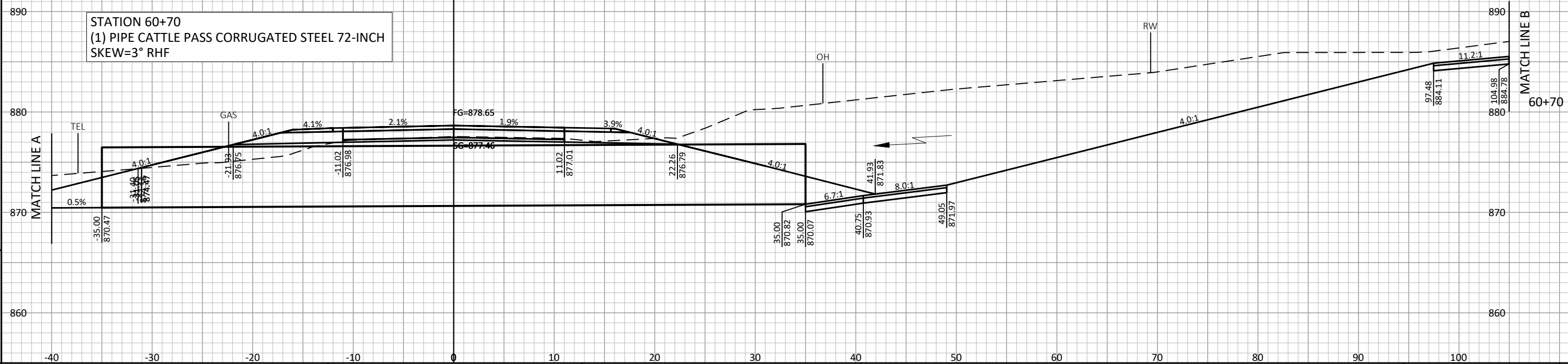
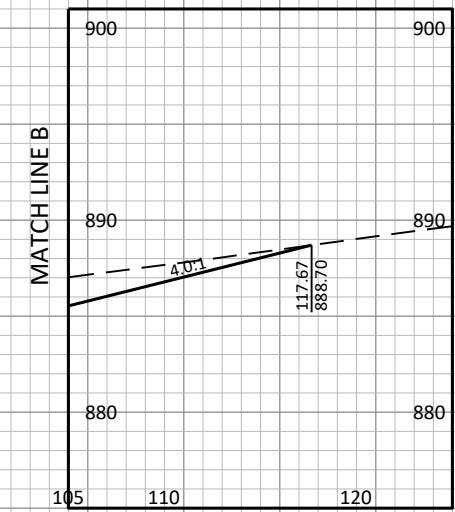
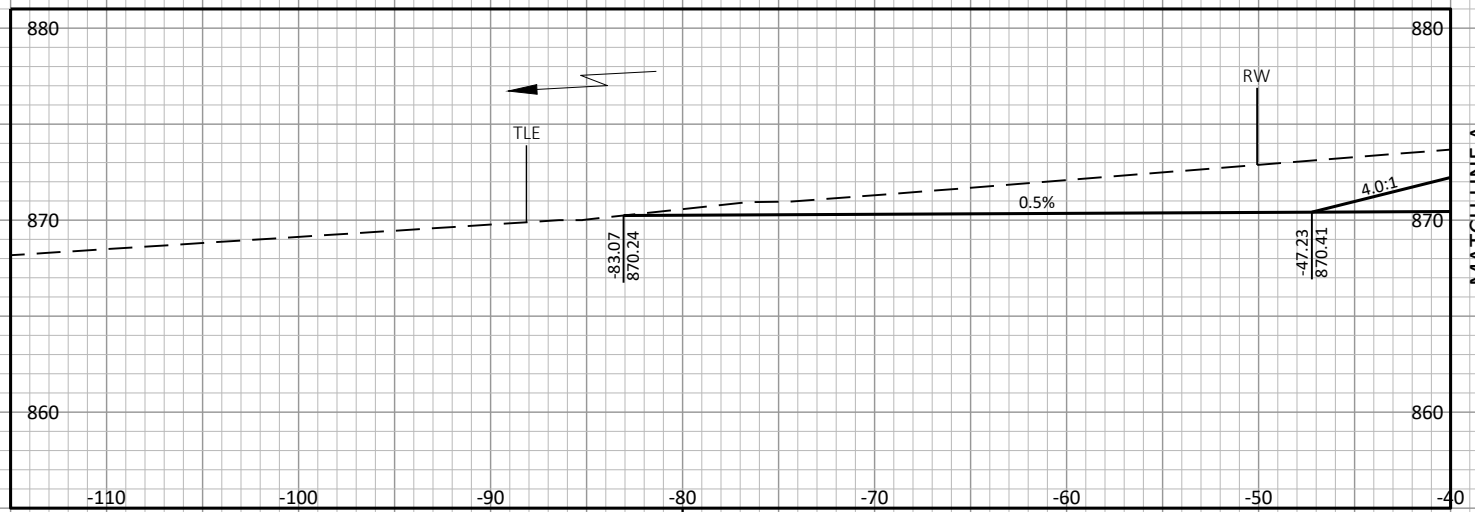
SHEET

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PROJECT NO: 5034-00-72

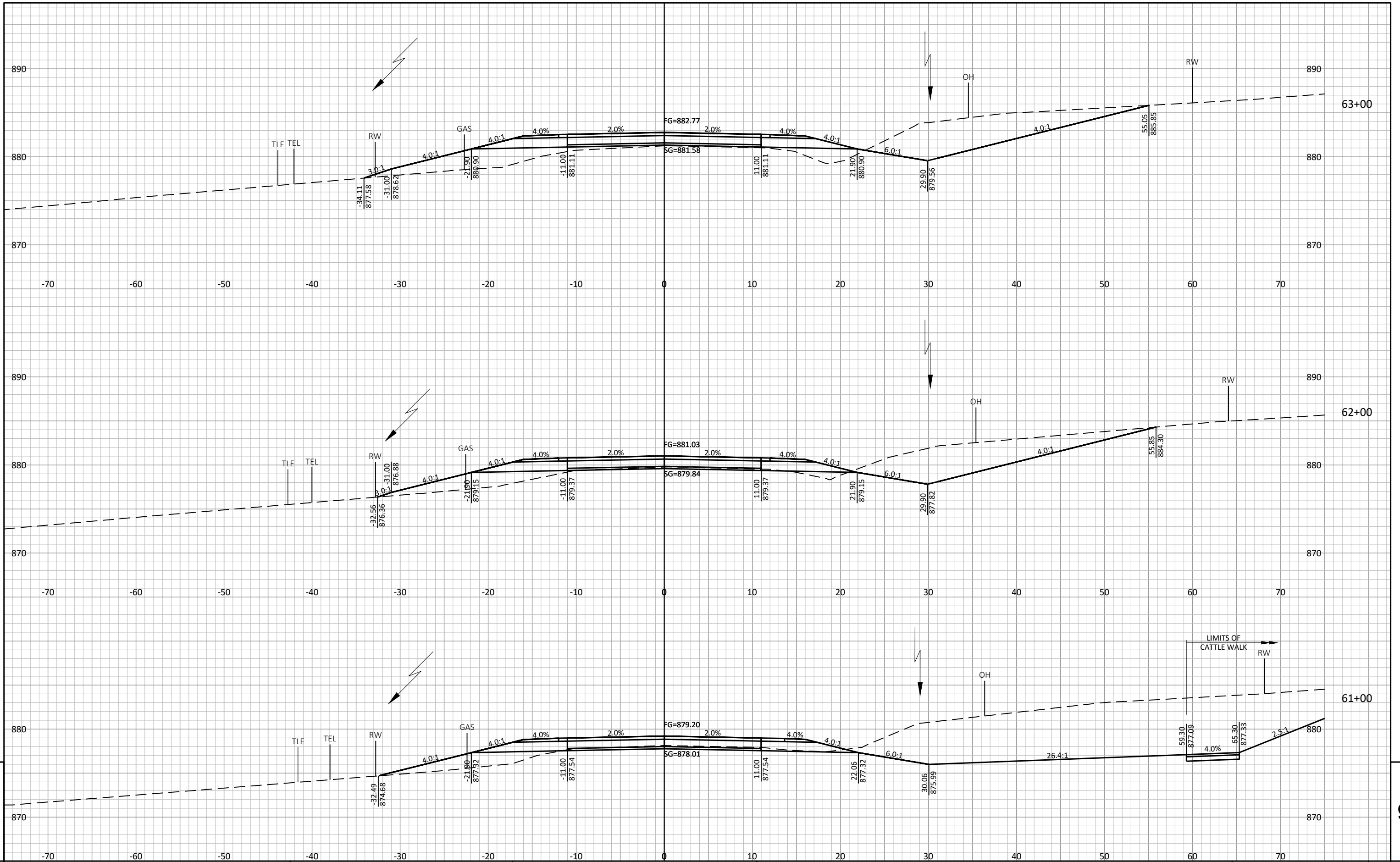
HWY: CTH F

COUNTY: IOWA

CROSS SECTIONS: MAINLINE

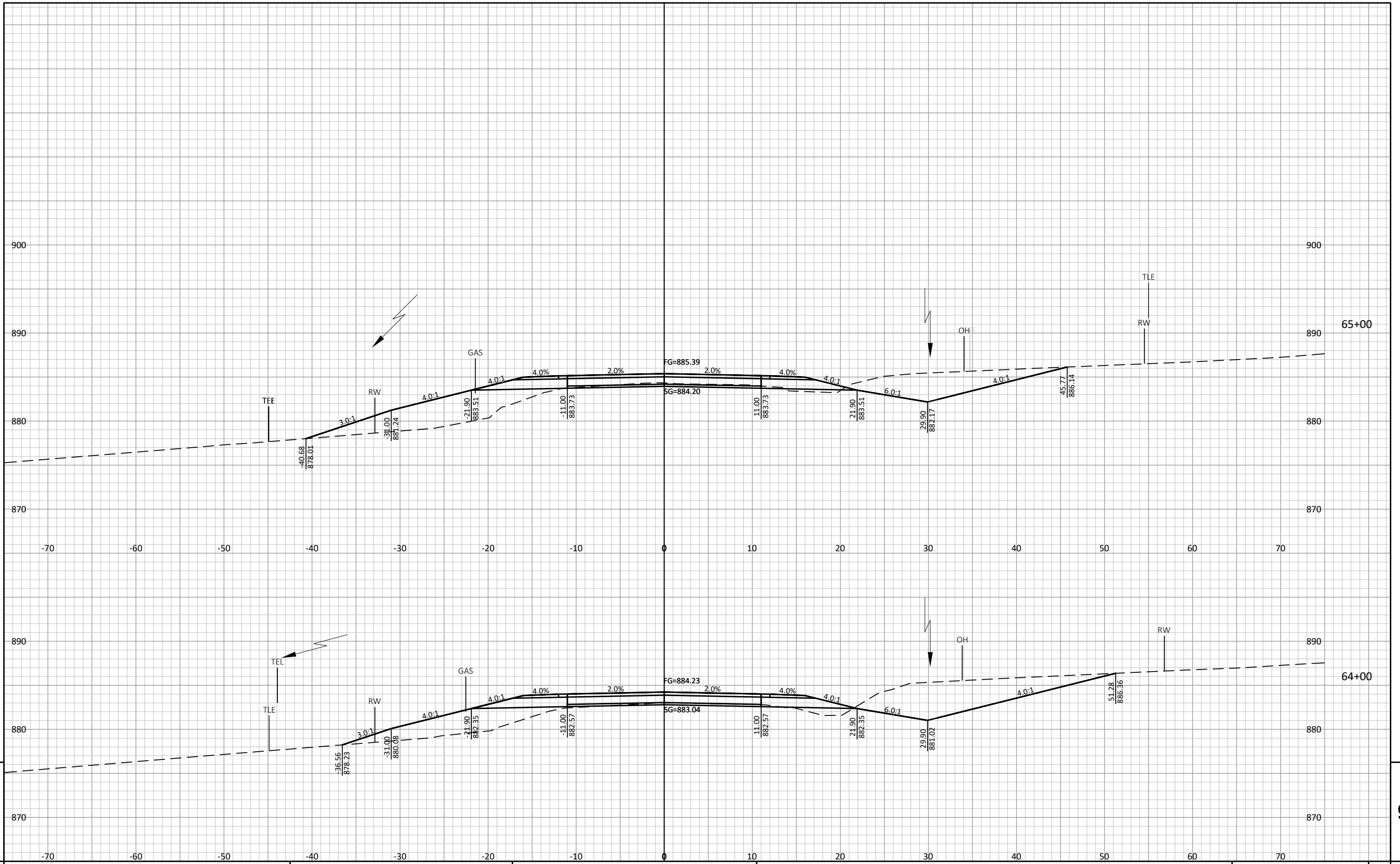
SHEET

E



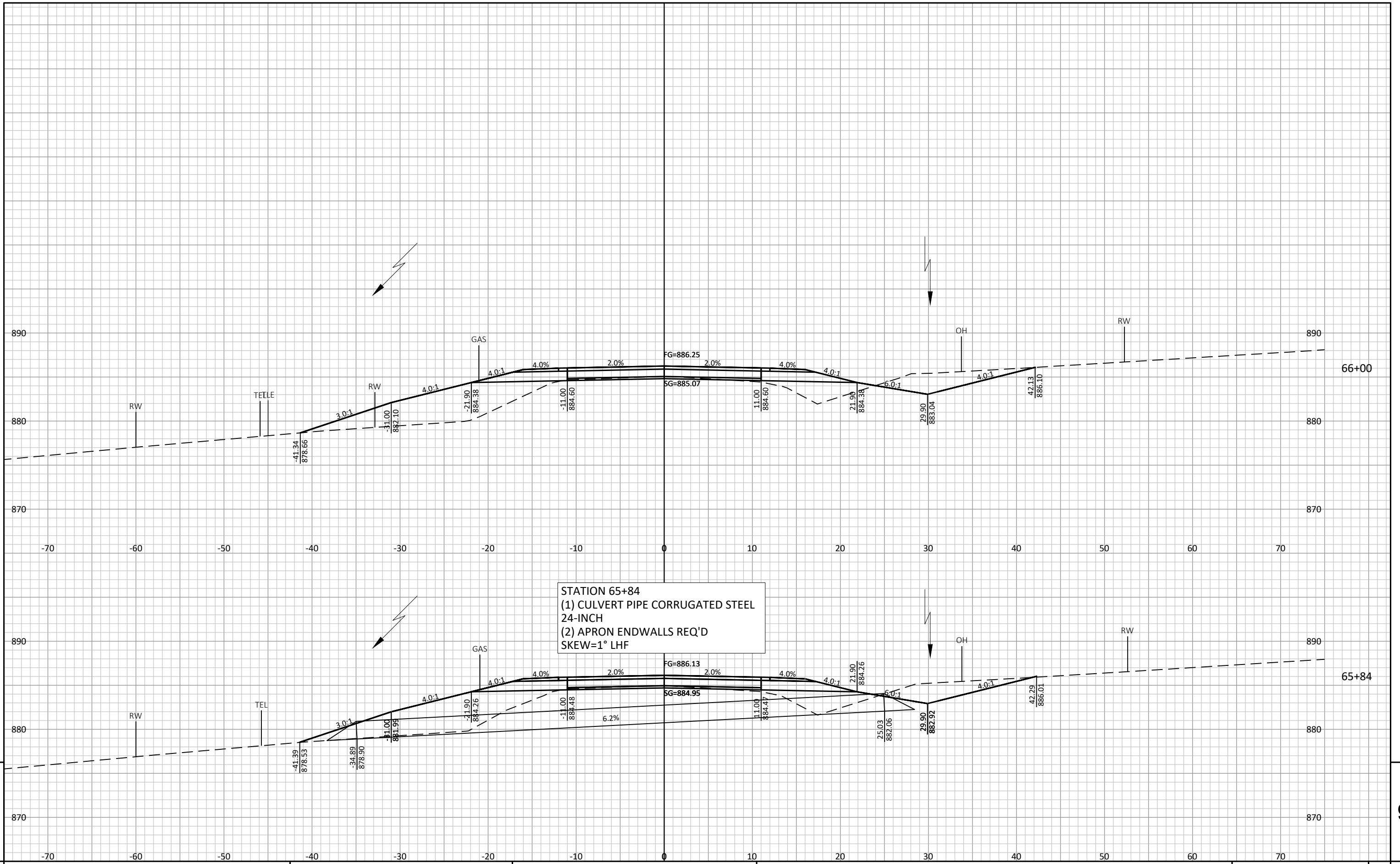
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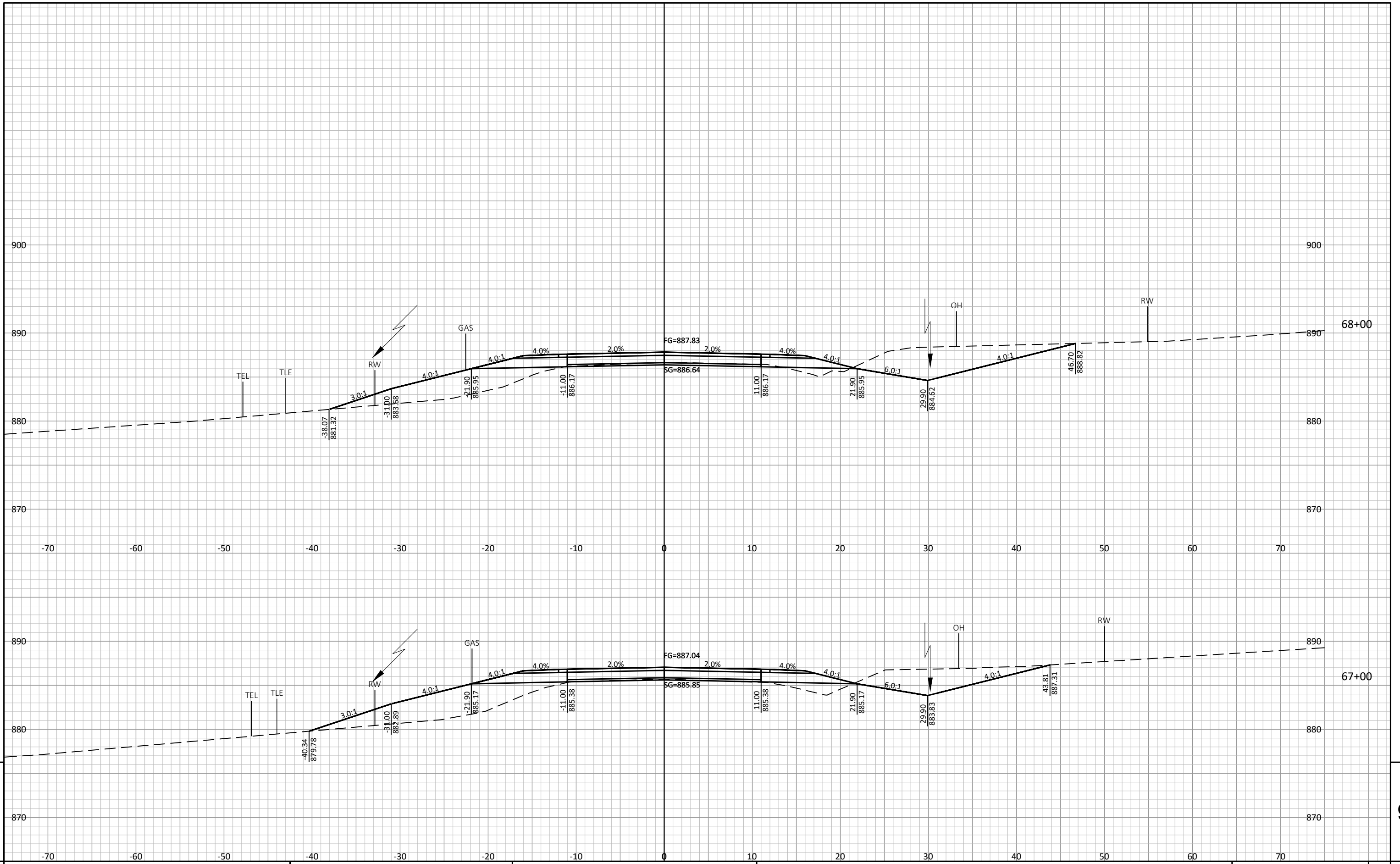
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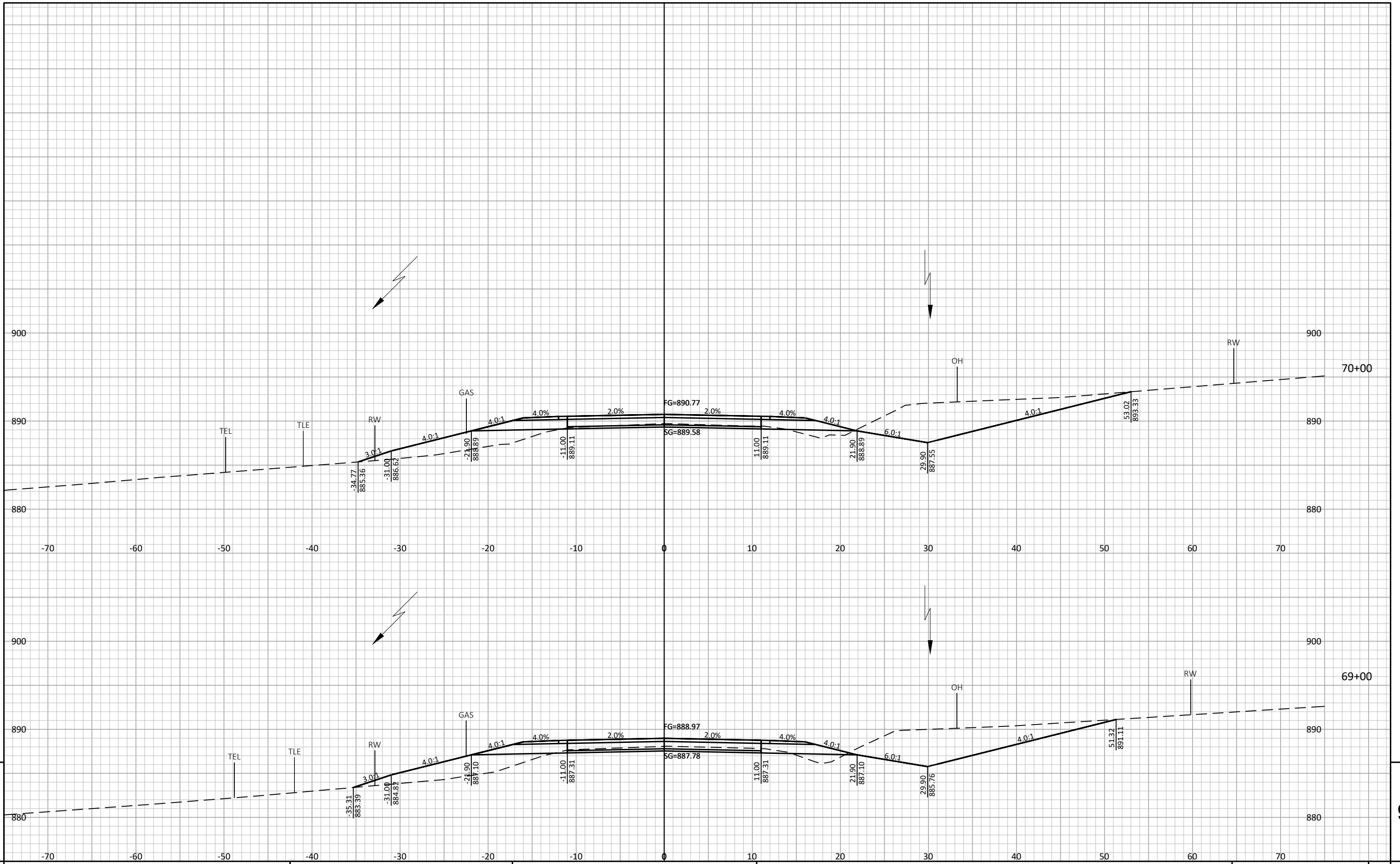
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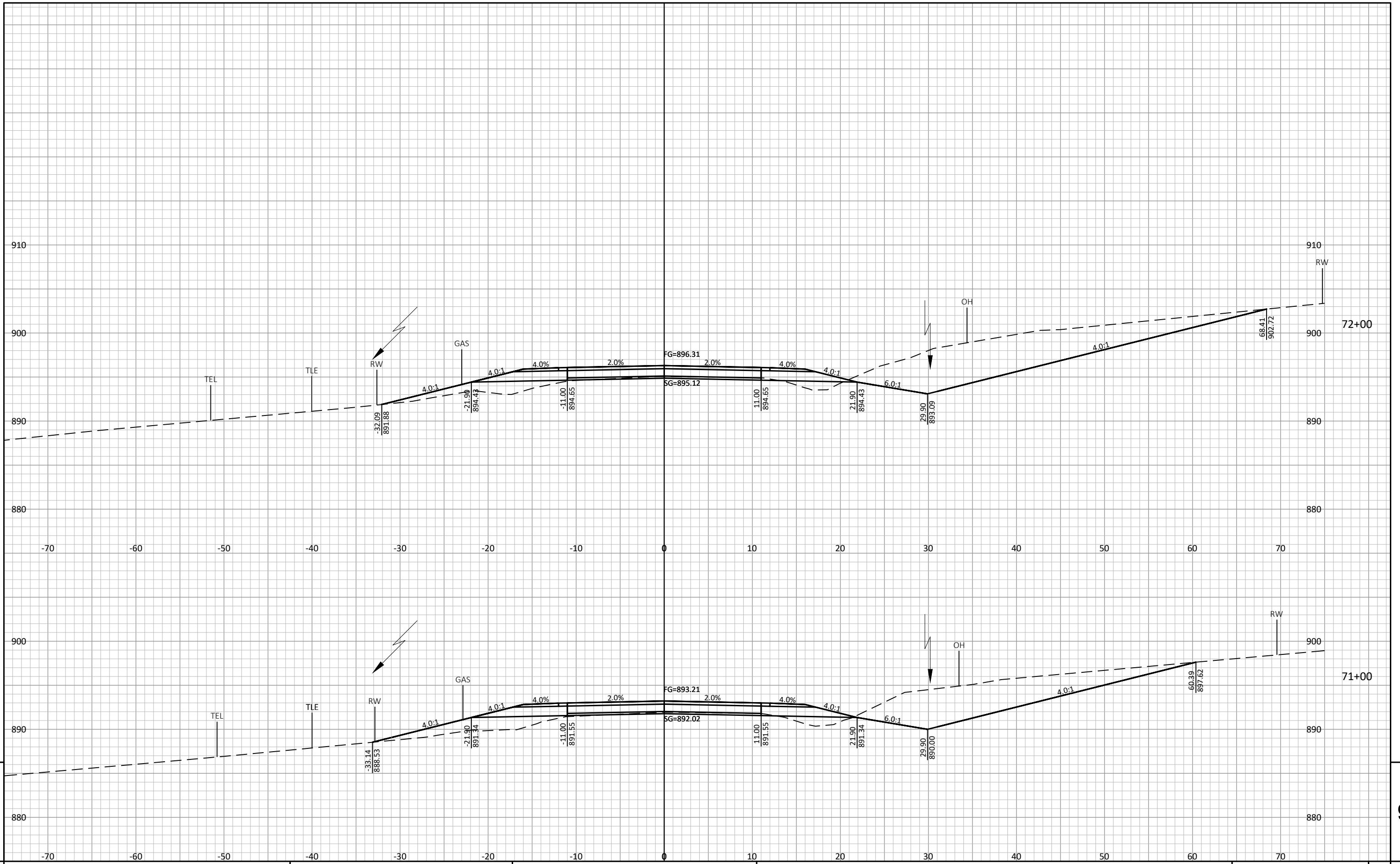


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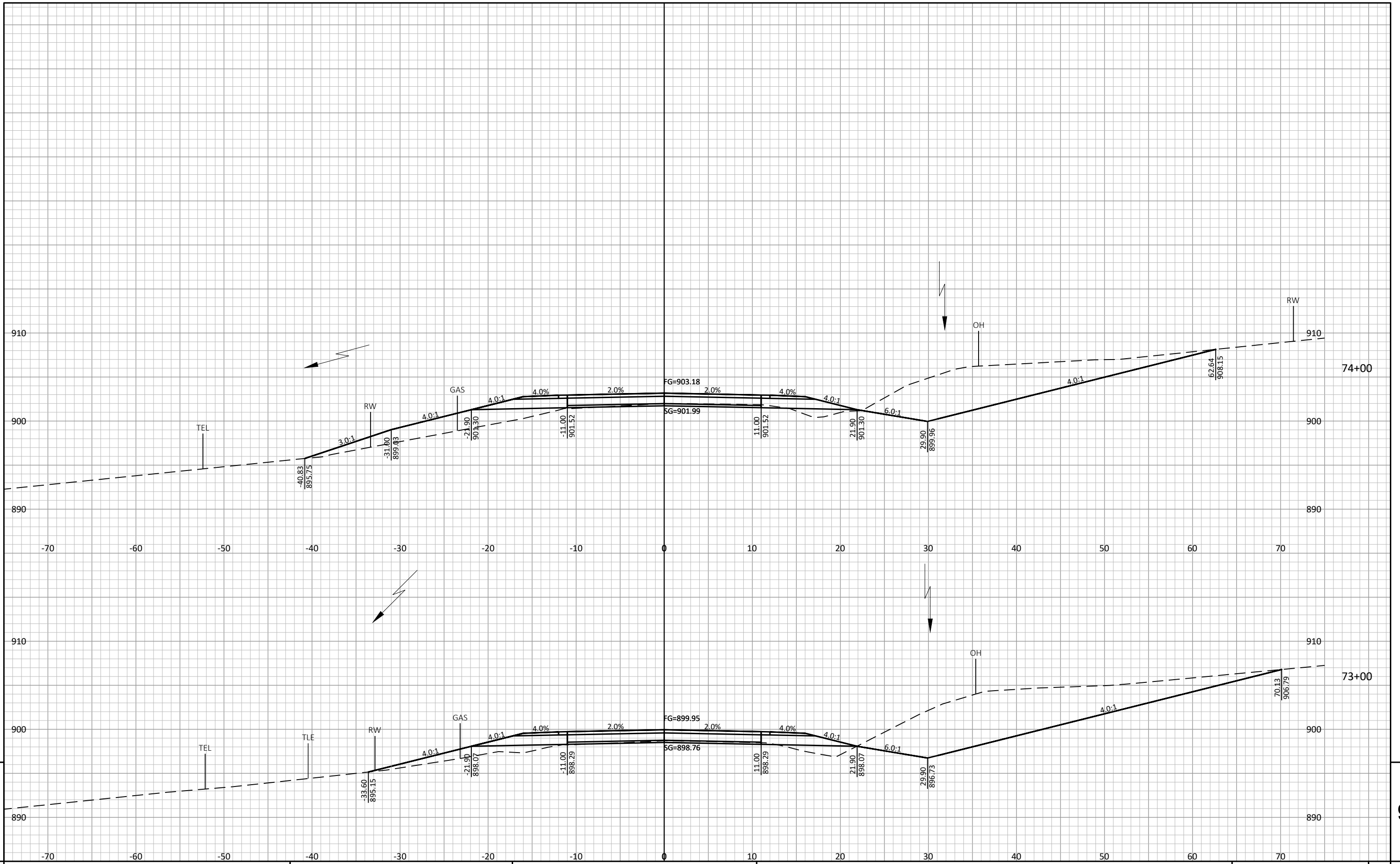
PROJECT NO: 5034-00-72 HWY: CTH F COUNTY: IOWA CROSS SECTIONS: MAINLINE SHEET E

FILE NAME : S:\PROJECTS\K19250 CTH F IOWA COUNTY\DESIGN\CORRIDORS\CORRIDOR_CTH F.DWG PLOT DATE : 9/8/2021 12:10:34 PM PLOT BY : JONAH DRAKE PLOT SCALE : 1" = 1' LAYOUT : CTH K 26



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PROJECT NO: 5034-00-72

HWY: CTH F

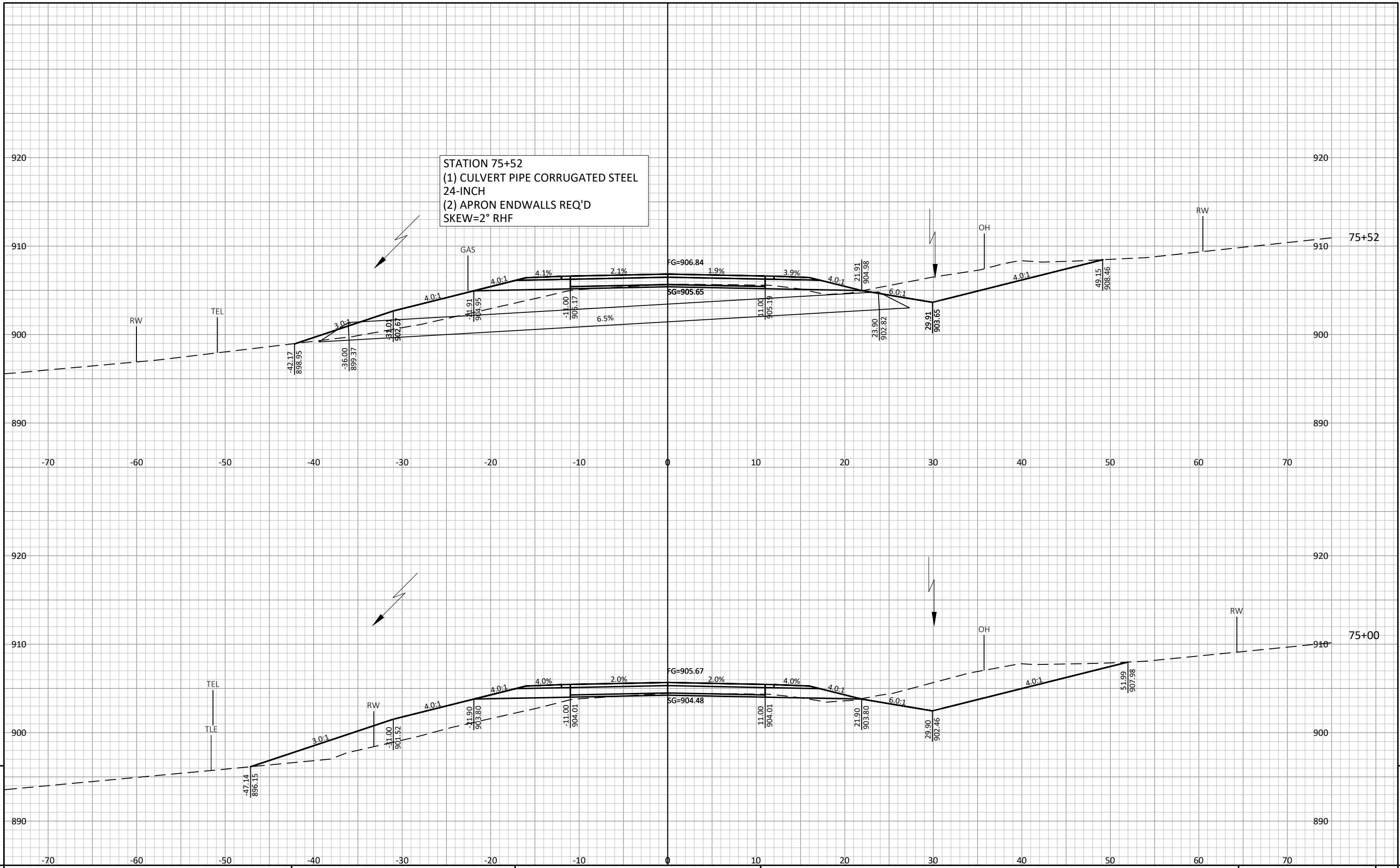
COUNTY: IOWA

CROSS SECTIONS: MAINLINE

SHEET

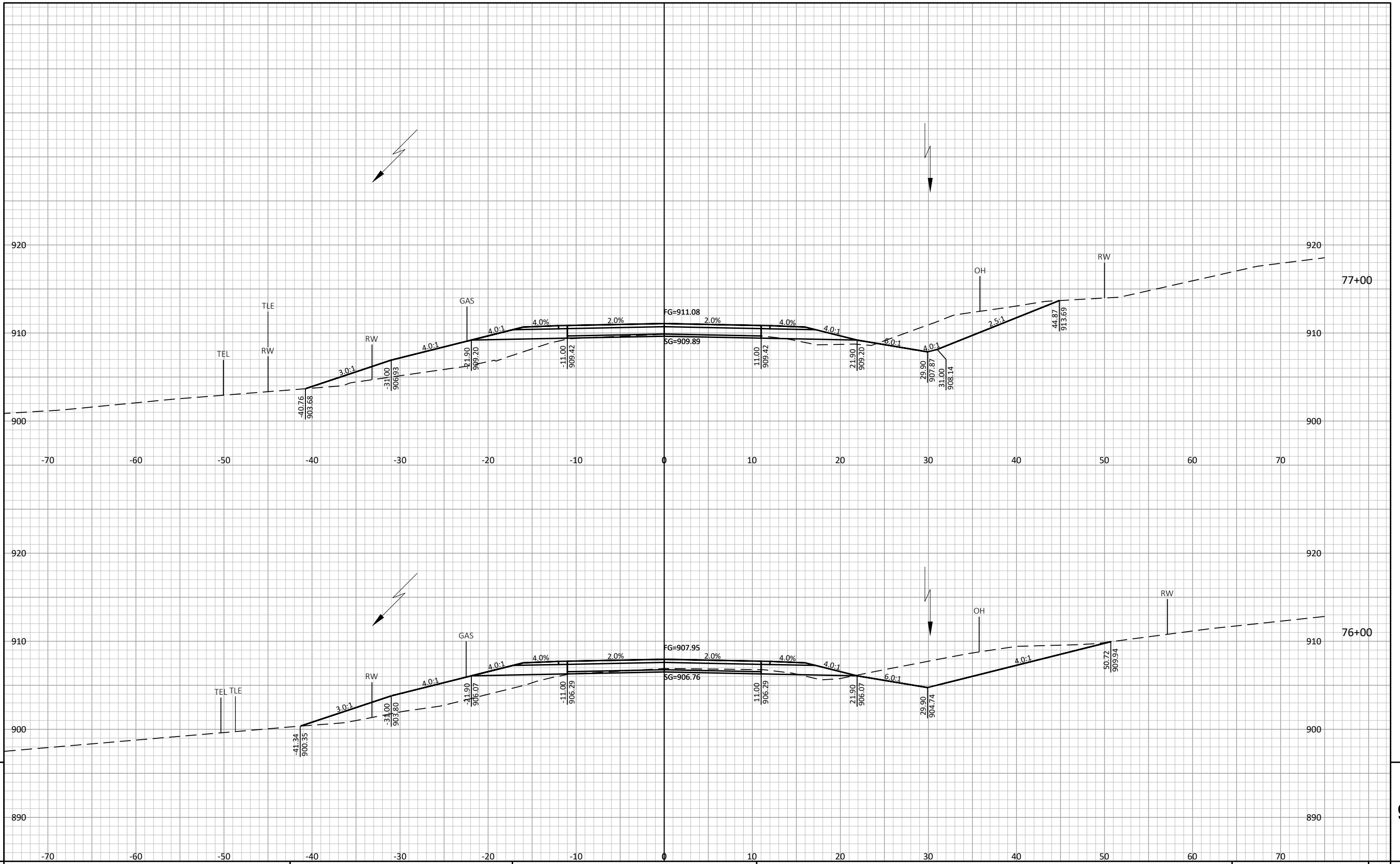
E

STATION 75+52
 (1) CULVERT PIPE CORRUGATED STEEL
 24-INCH
 (2) APRON ENDWALLS REQ'D
 SKEW=2° RHF



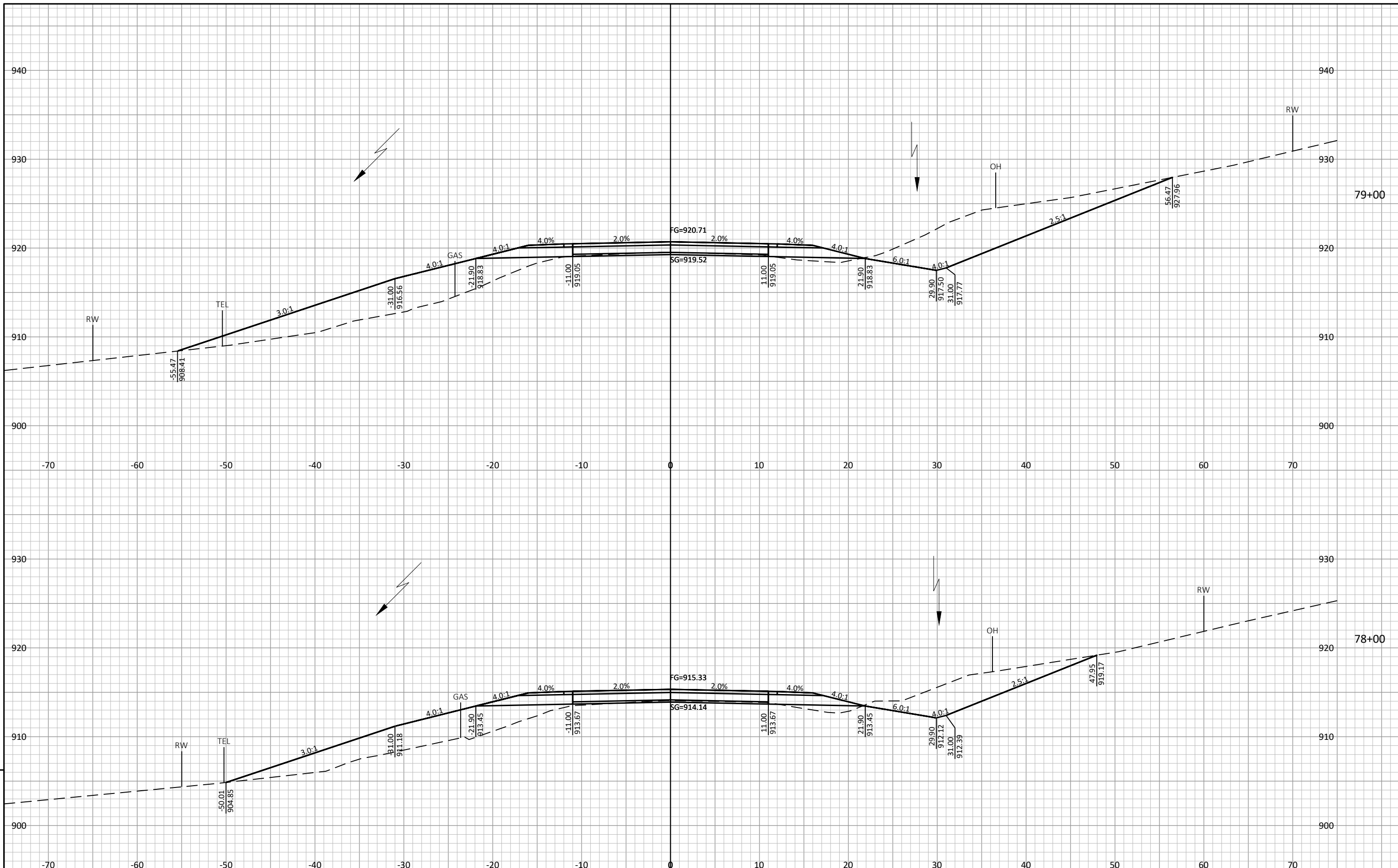
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PROJECT NO: 5034-00-72

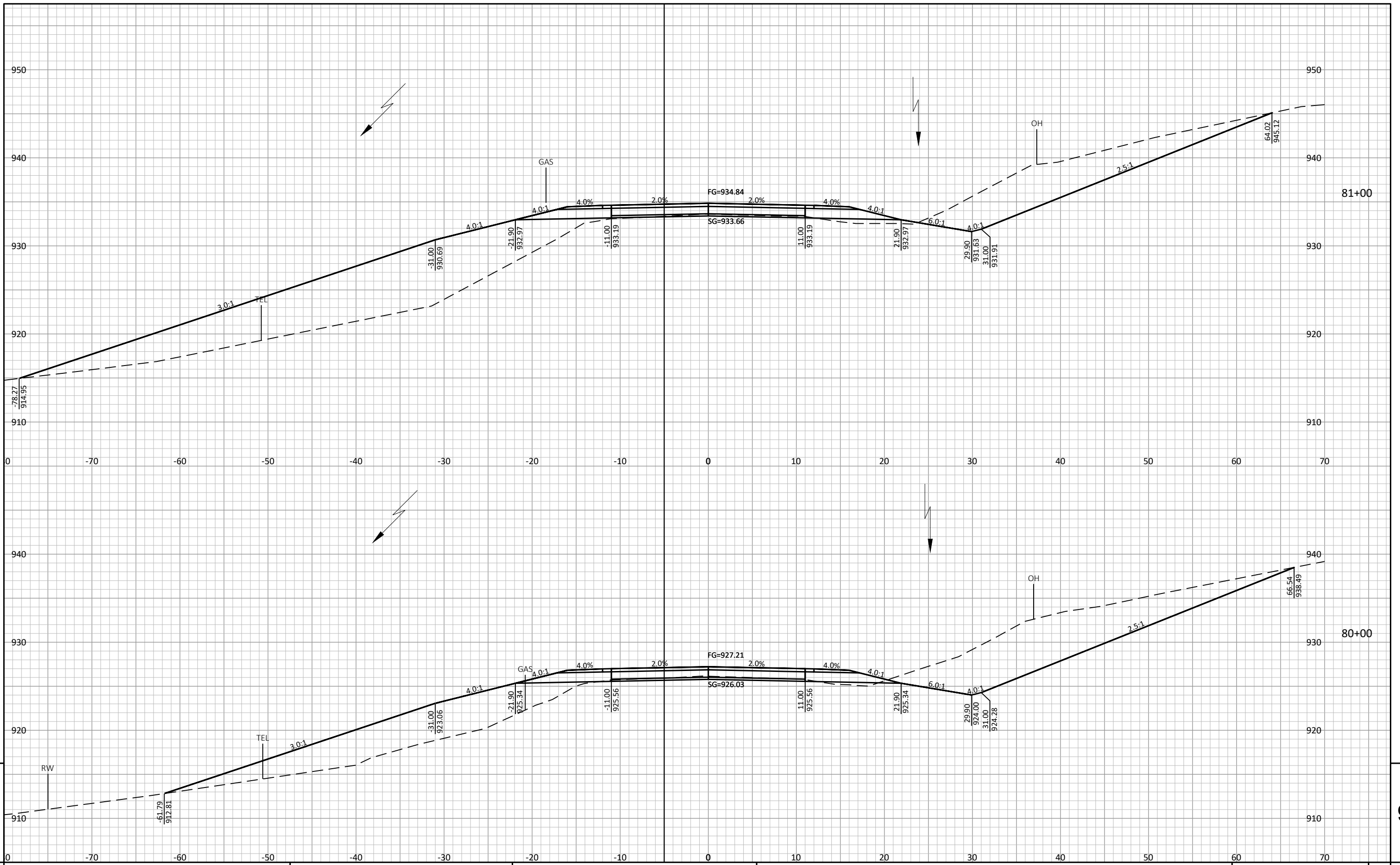
HWY: CTH F

COUNTY: IOWA

CROSS SECTIONS: MAINLINE

SHEET

E



PROJECT NO: 5034-00-72

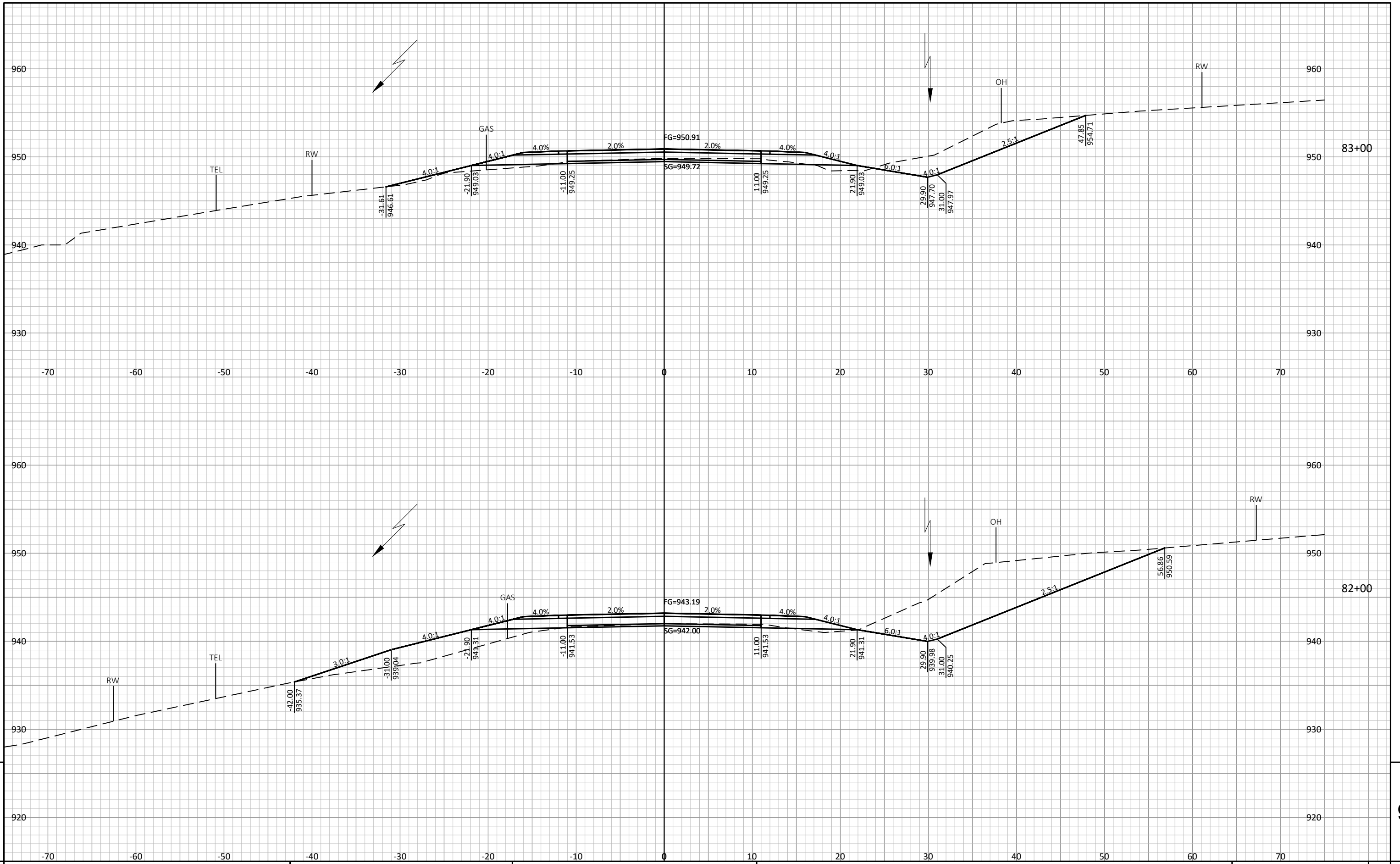
HWY: CTH F

COUNTY: IOWA

CROSS SECTIONS: MAINLINE

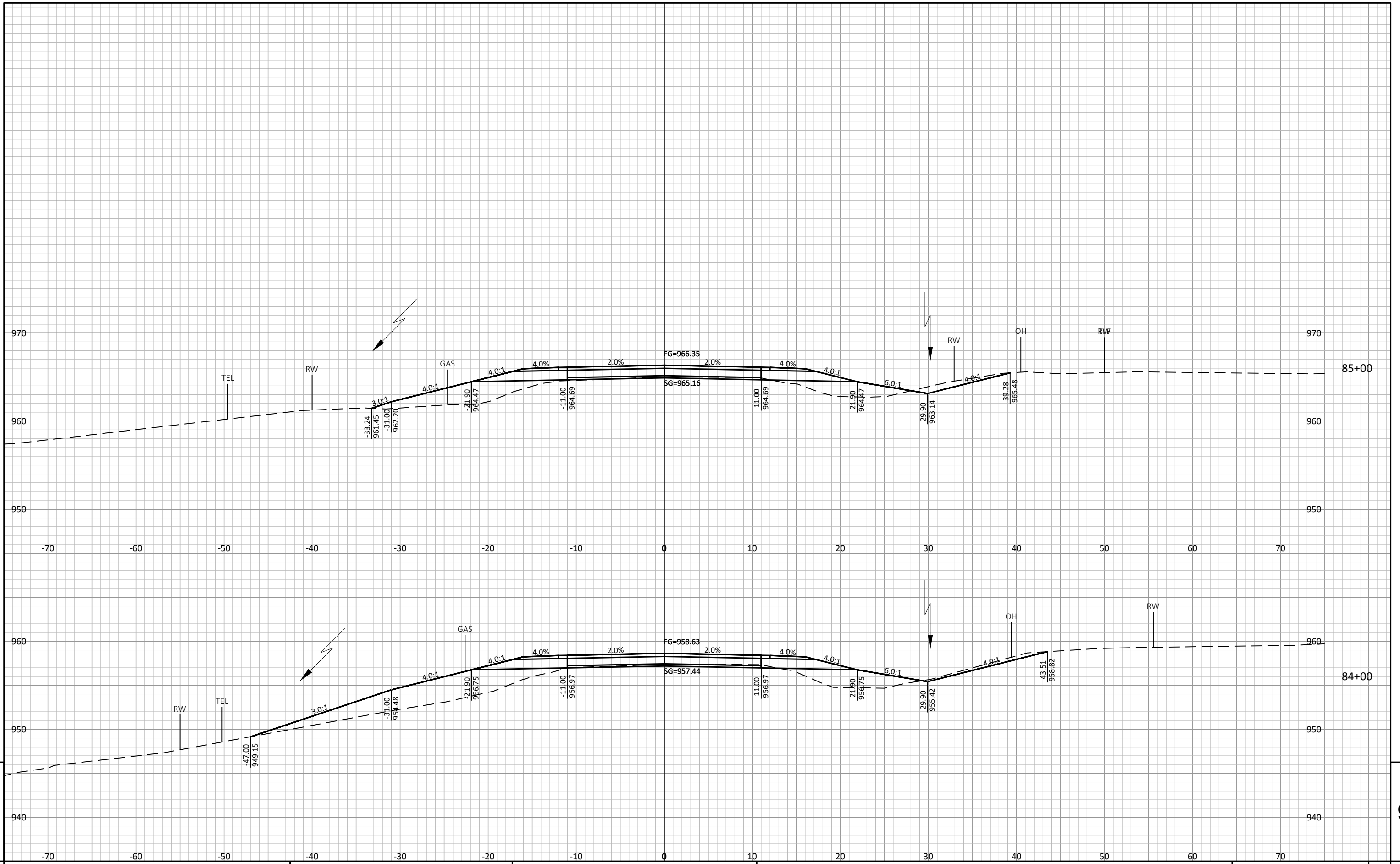
SHEET

E



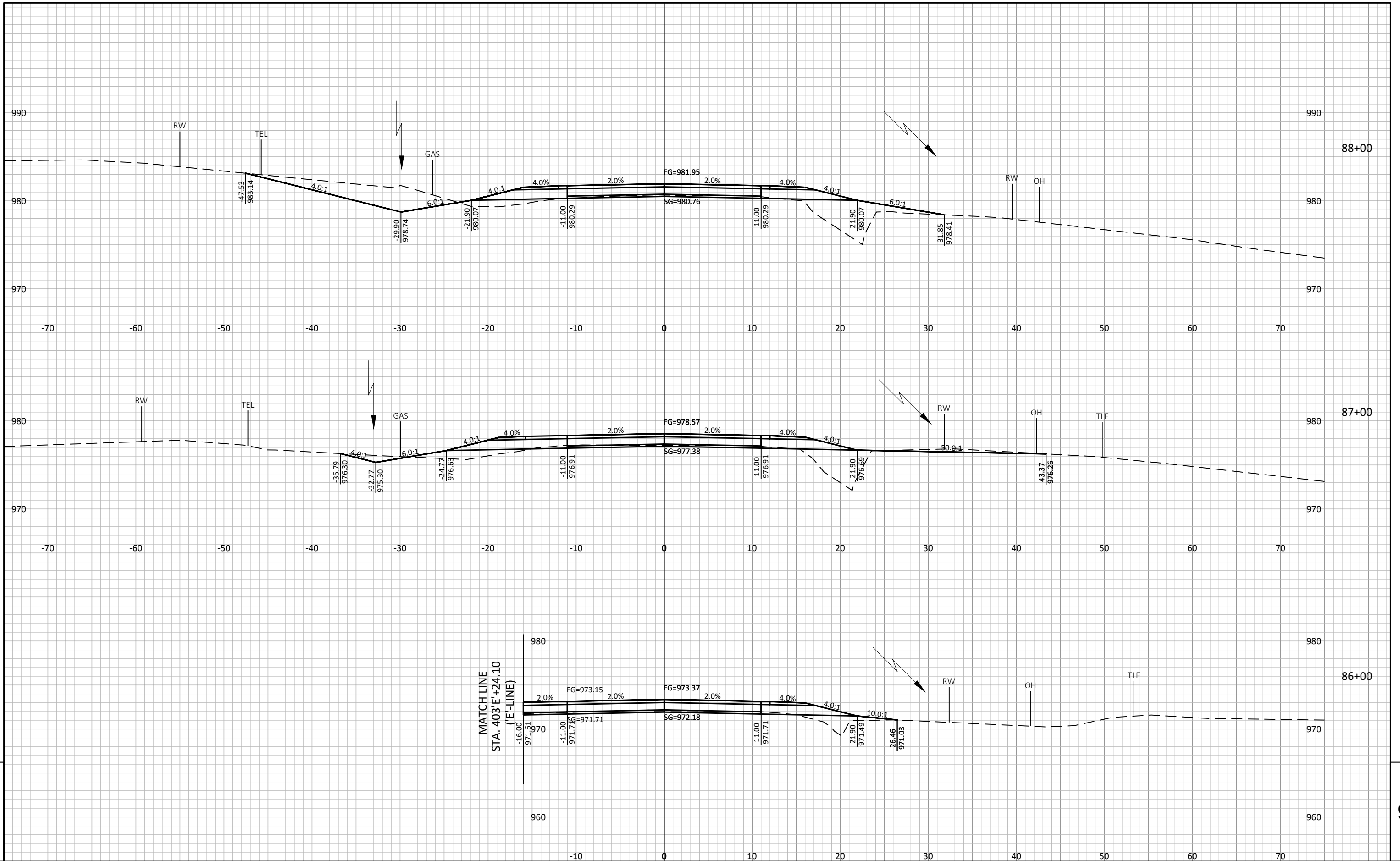
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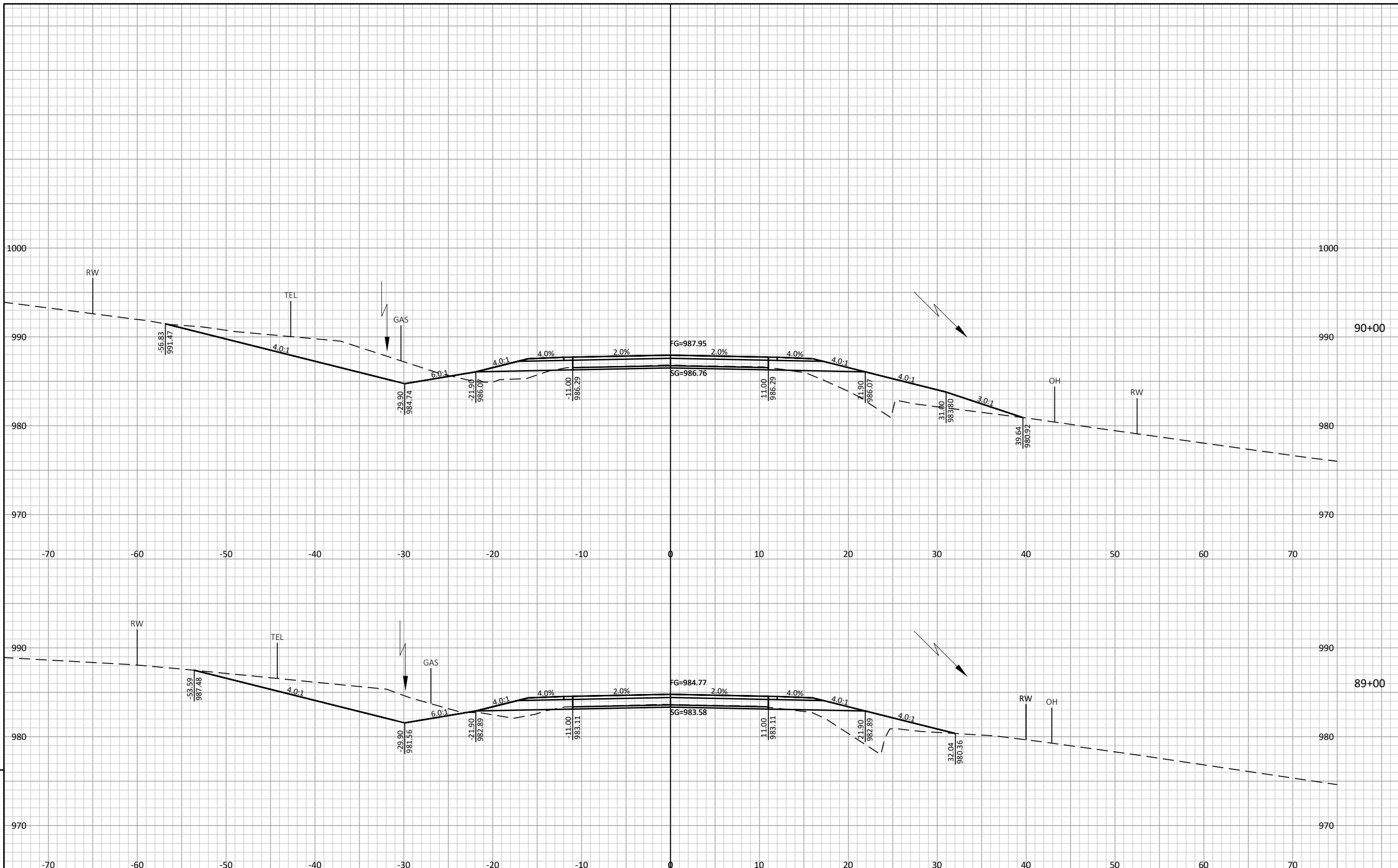
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PROJECT NO: 5034-00-72

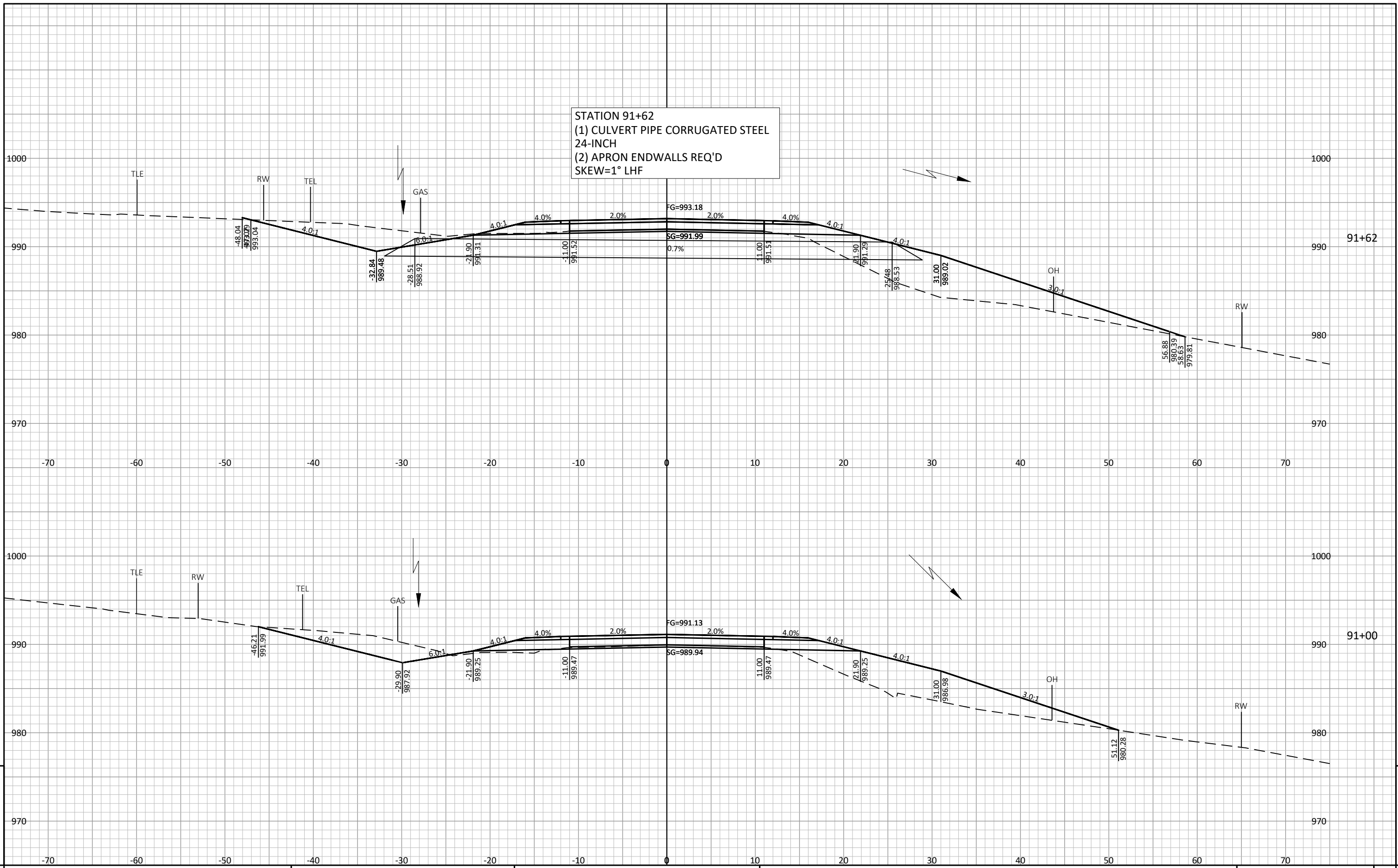
HWY: CTH F

COUNTY: IOWA

CROSS SECTIONS: MAINLINE

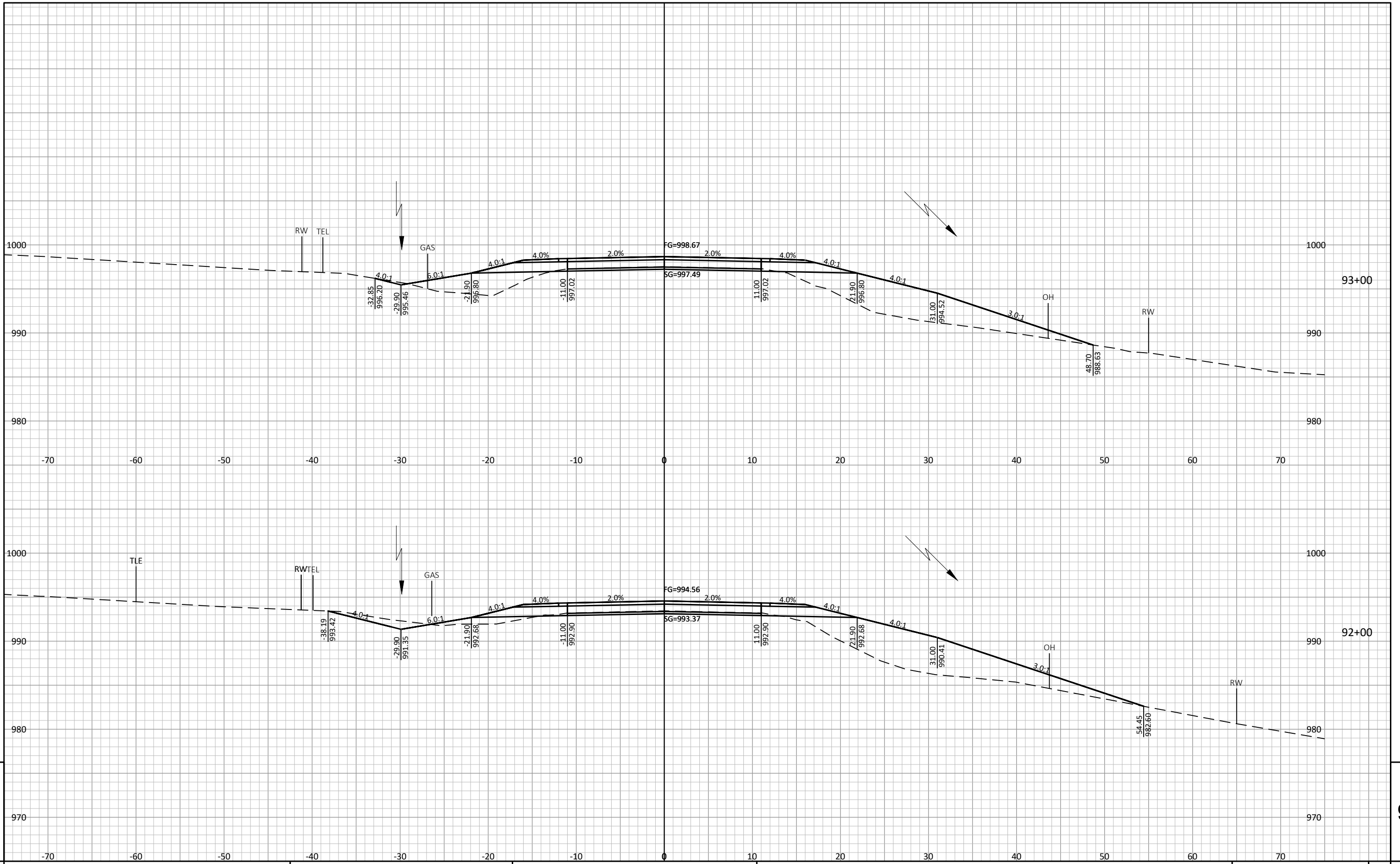
SHEET

E



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PROJECT NO: 5034-00-72

HWY: CTH F

COUNTY: IOWA

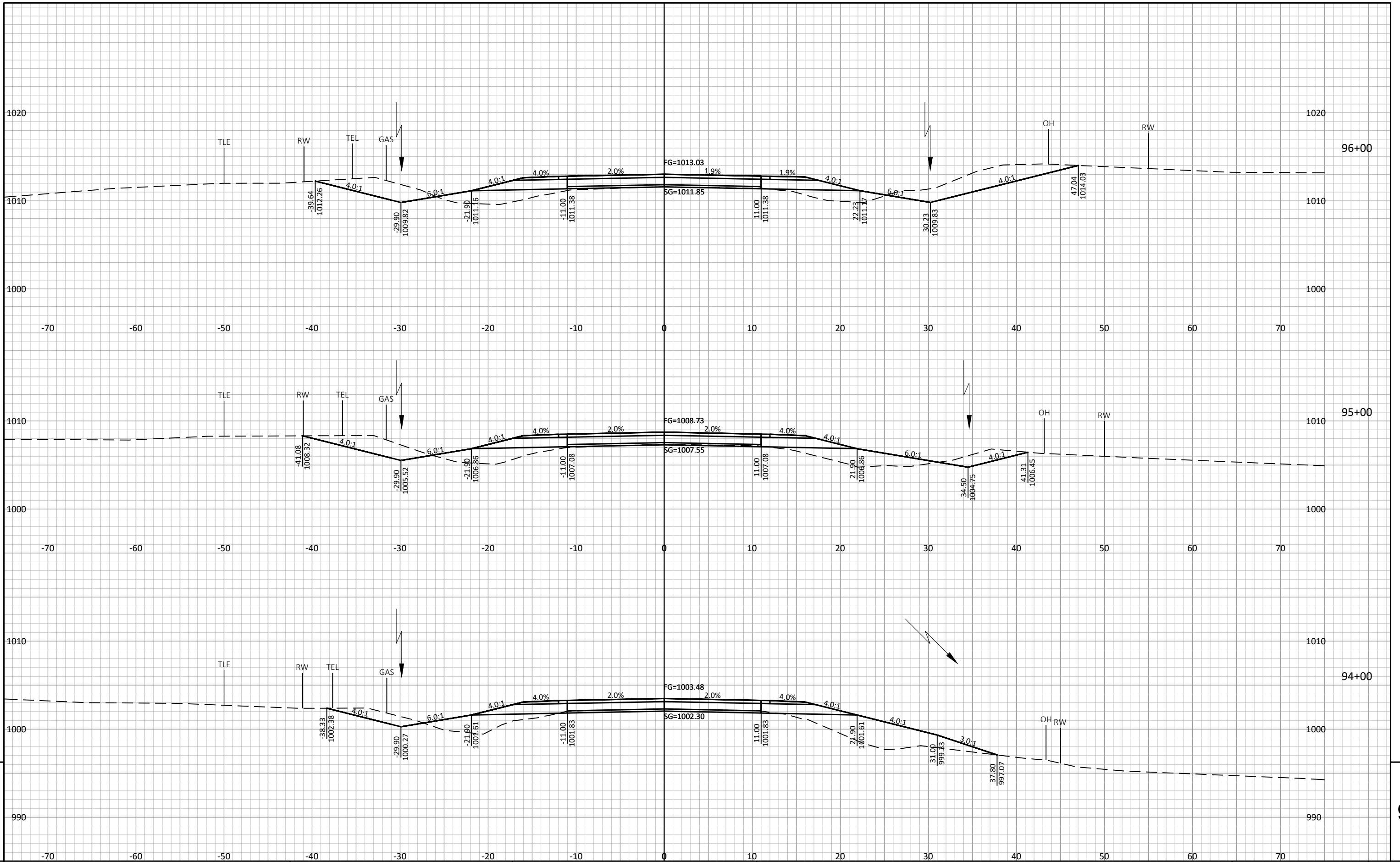
CROSS SECTIONS: MAINLINE

SHEET

E

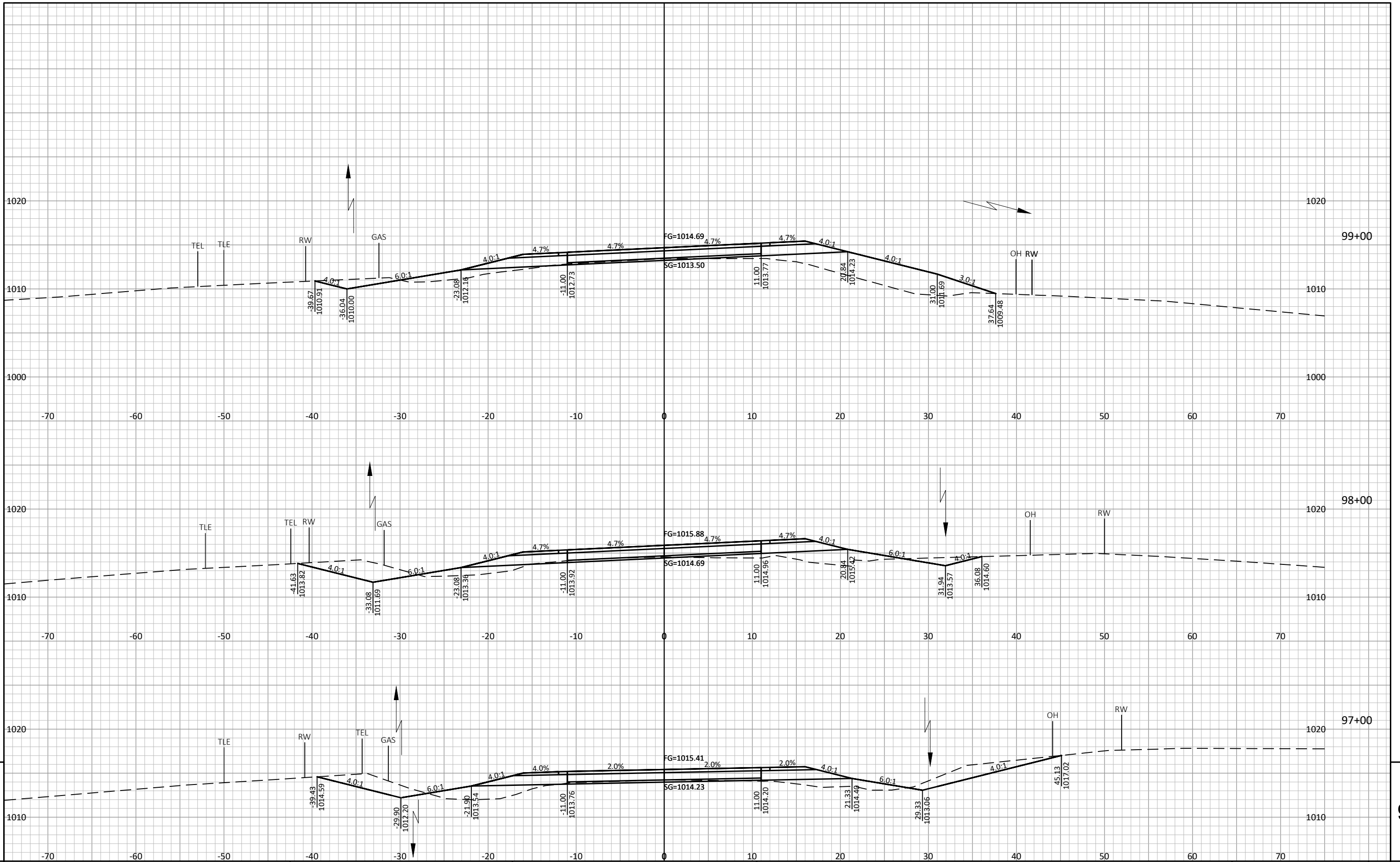
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PROJECT NO: 5034-00-72

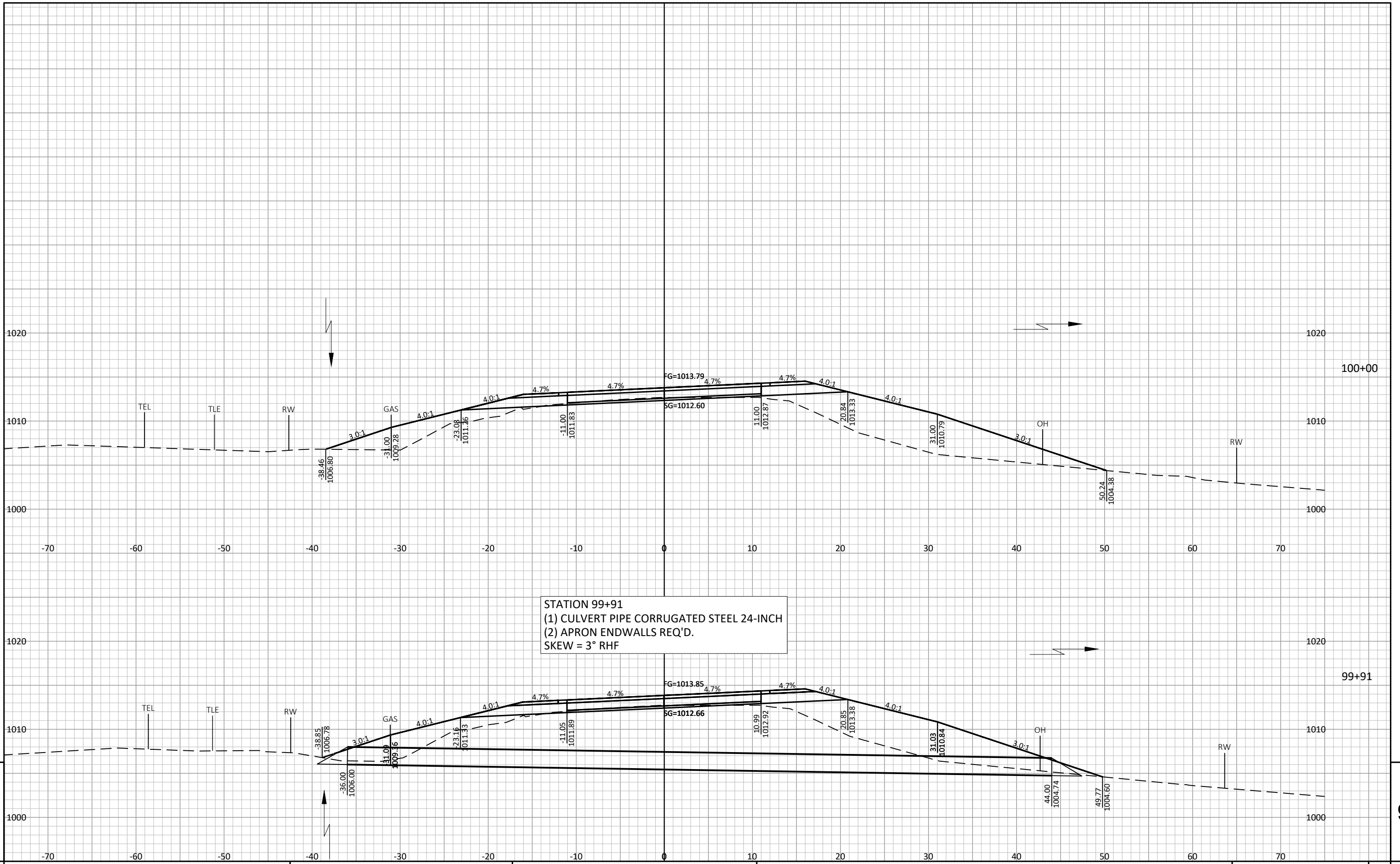
HWY: CTH F

COUNTY: IOWA

CROSS SECTIONS: MAINLINE

SHEET

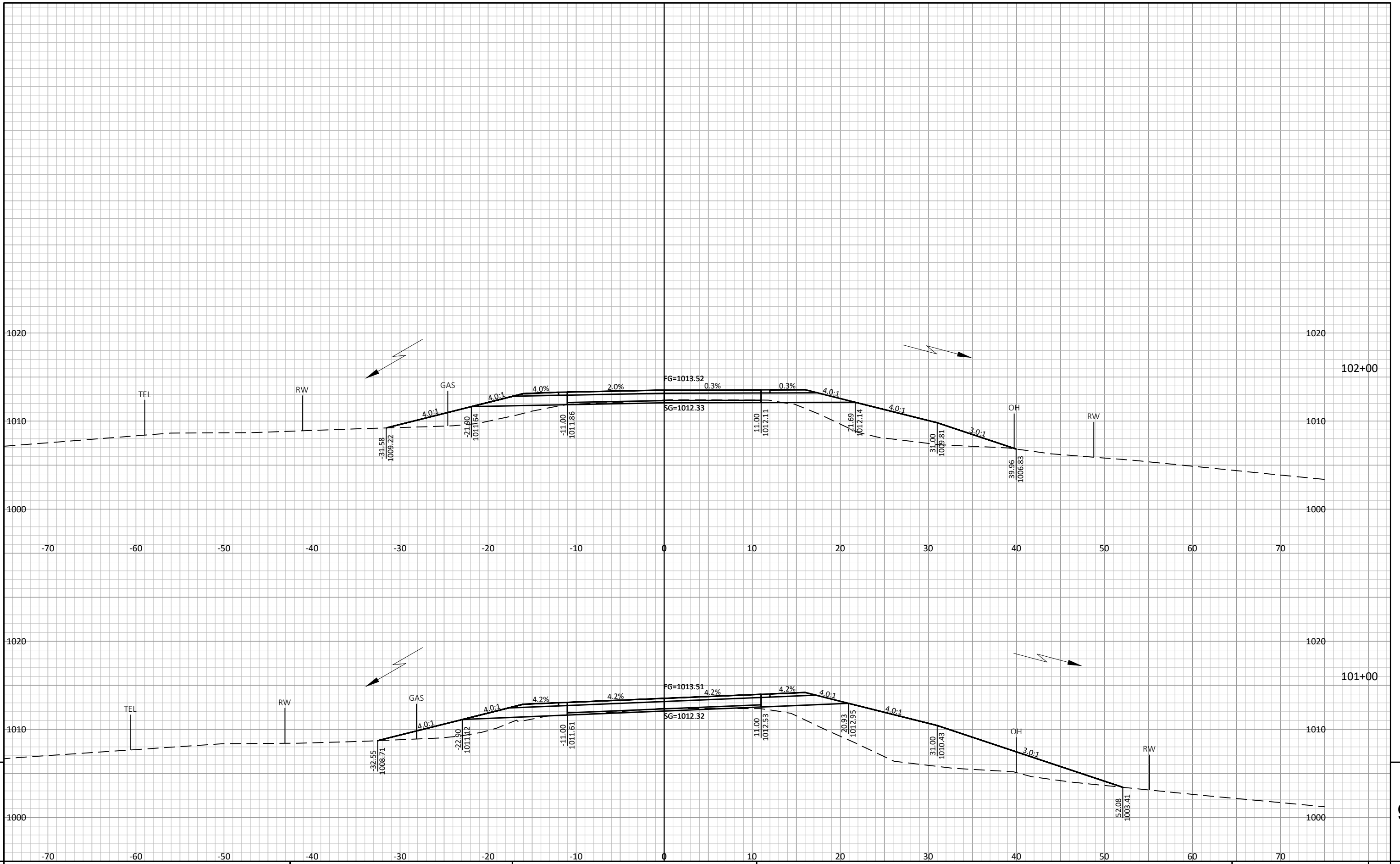
E



STATION 99+91
 (1) CULVERT PIPE CORRUGATED STEEL 24-INCH
 (2) APRON ENDWALLS REQ'D.
 SKEW = 3° RHF

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PROJECT NO: 5034-00-72

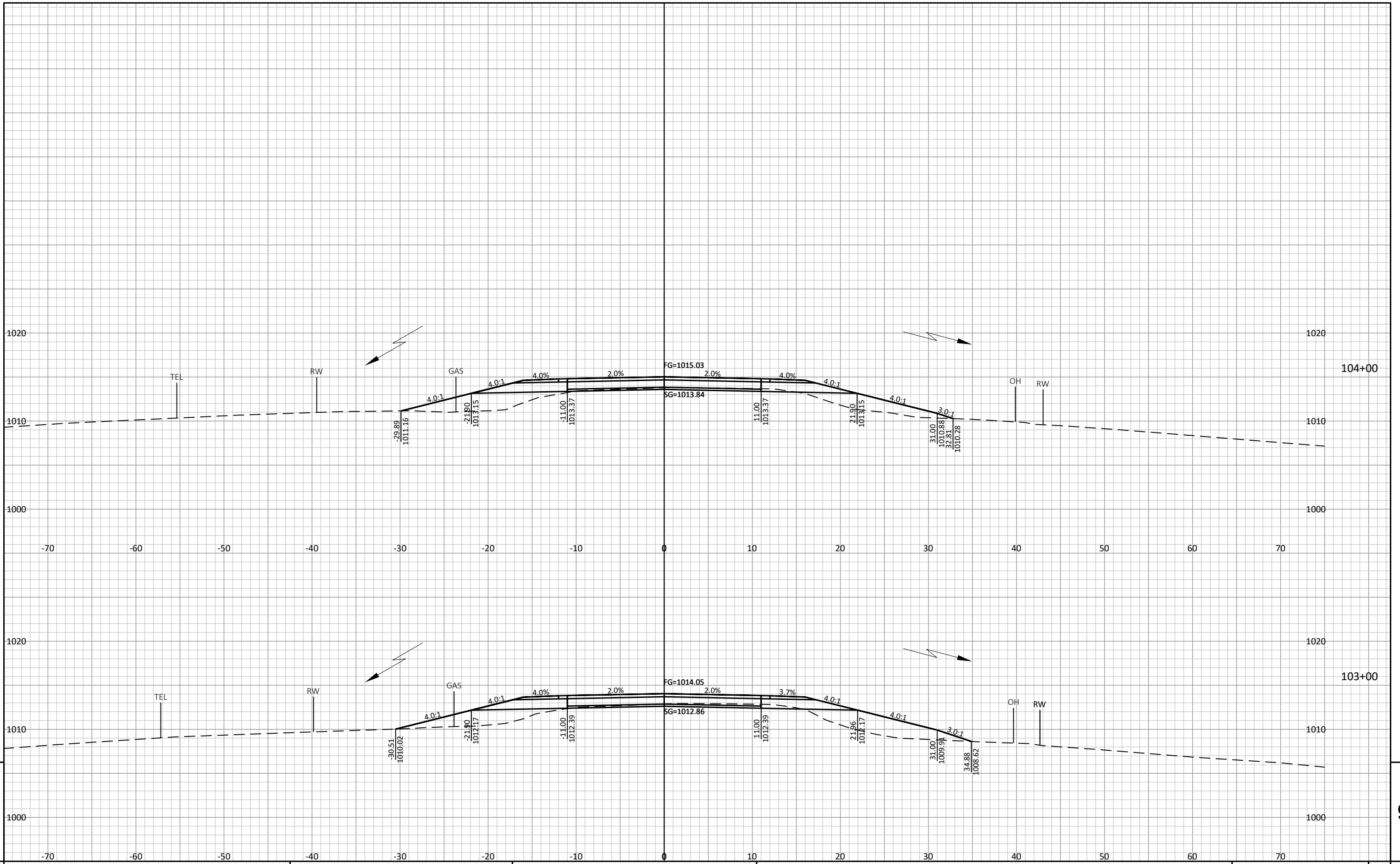
HWY: CTH F

COUNTY: IOWA

CROSS SECTIONS: MAINLINE

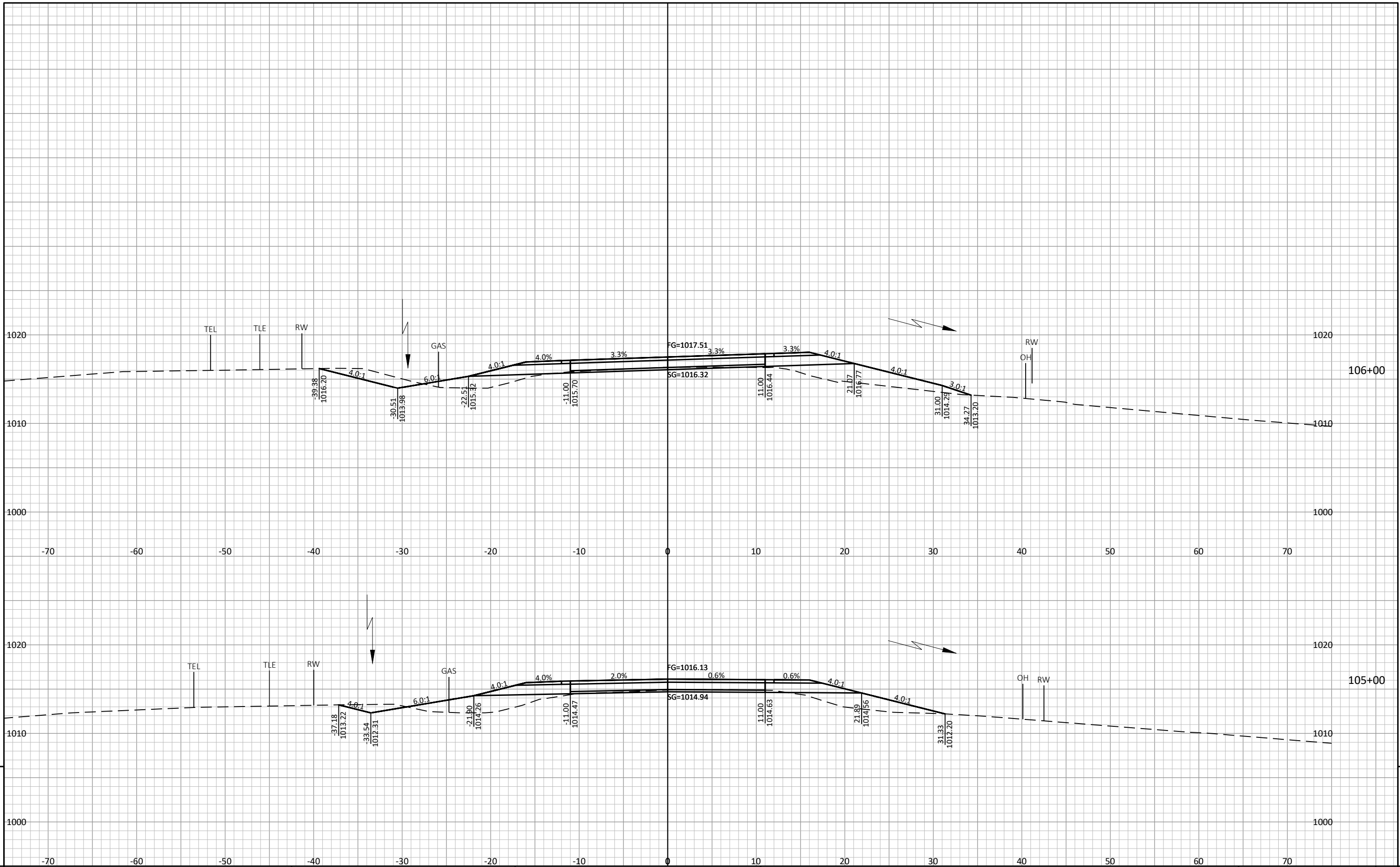
SHEET

E



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PROJECT NO: 5034-00-72

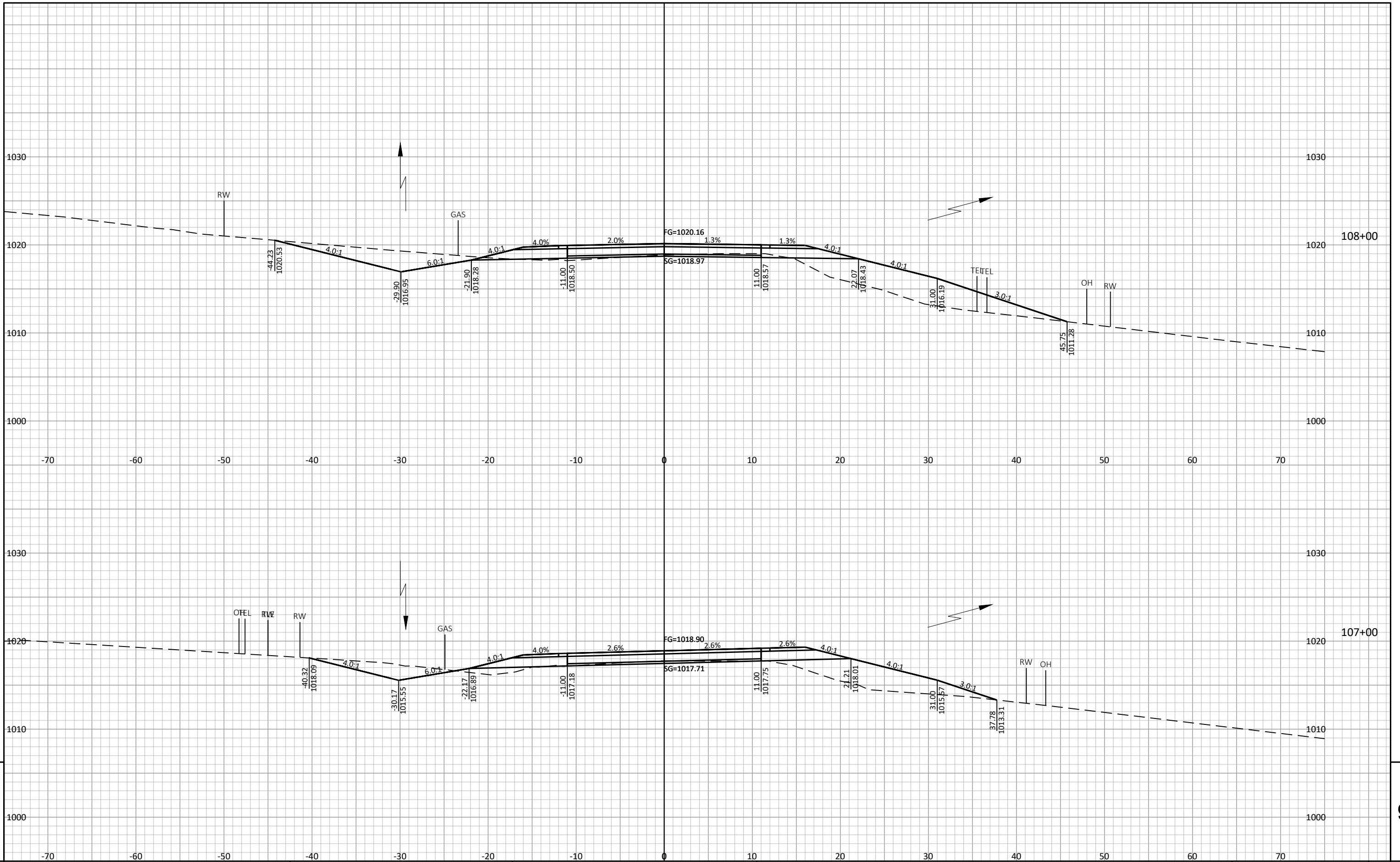
HWY: CTH F

COUNTY: IOWA

CROSS SECTIONS: MAINLINE

SHEET

E



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PROJECT NO: 5034-00-72

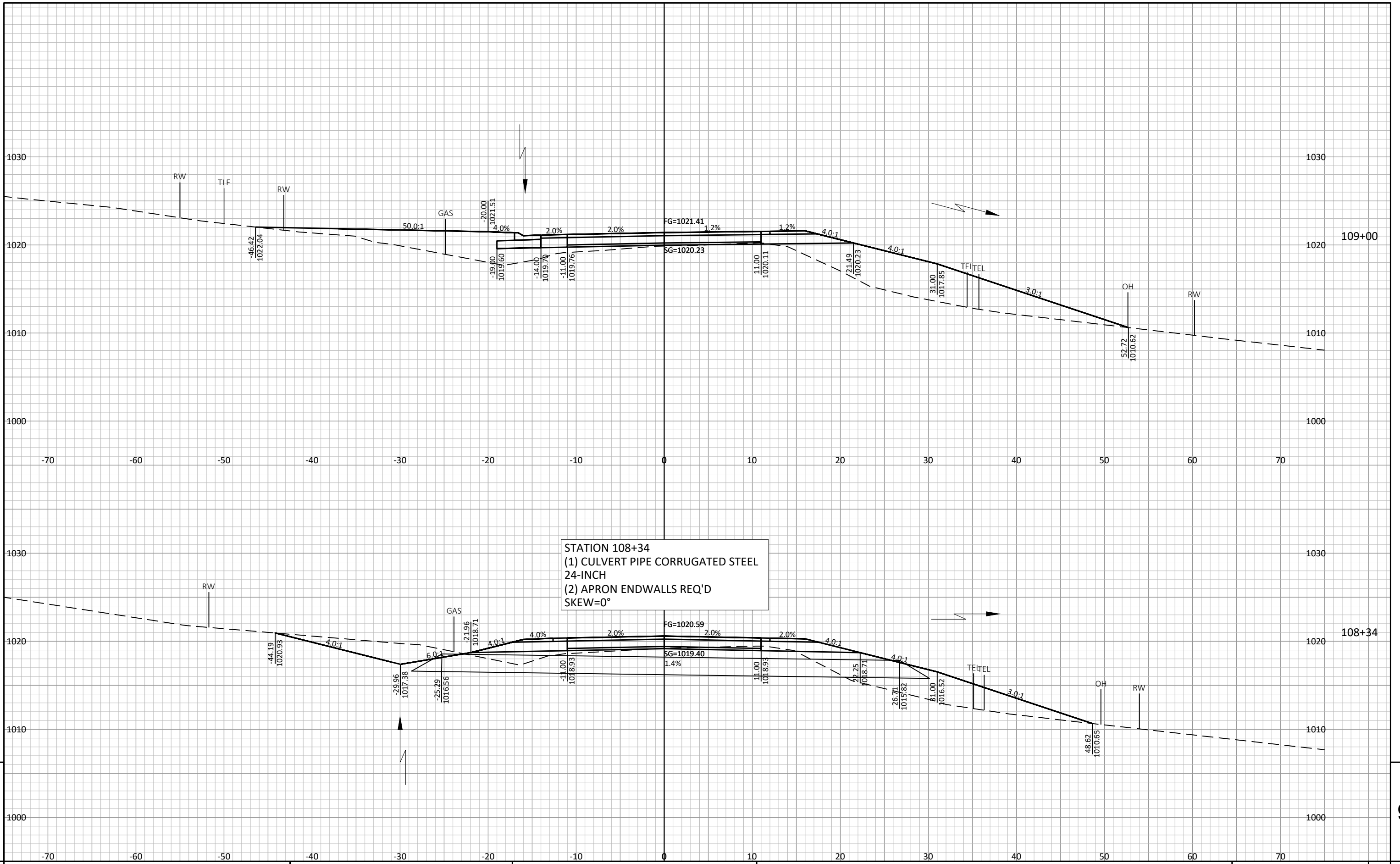
HWY: CTH F

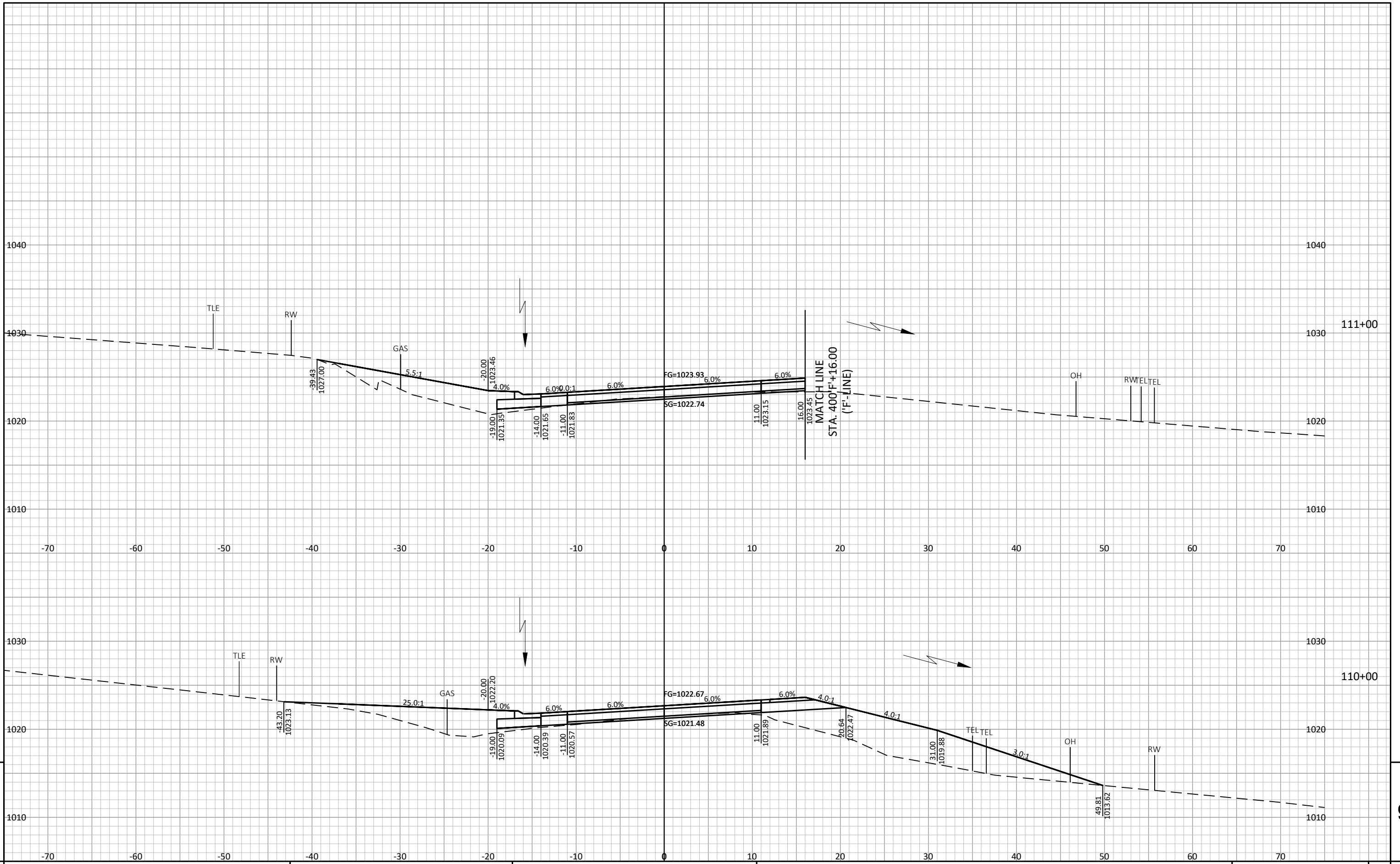
COUNTY: IOWA

CROSS SECTIONS: MAINLINE

SHEET

E





PROJECT NO: 5034-00-72

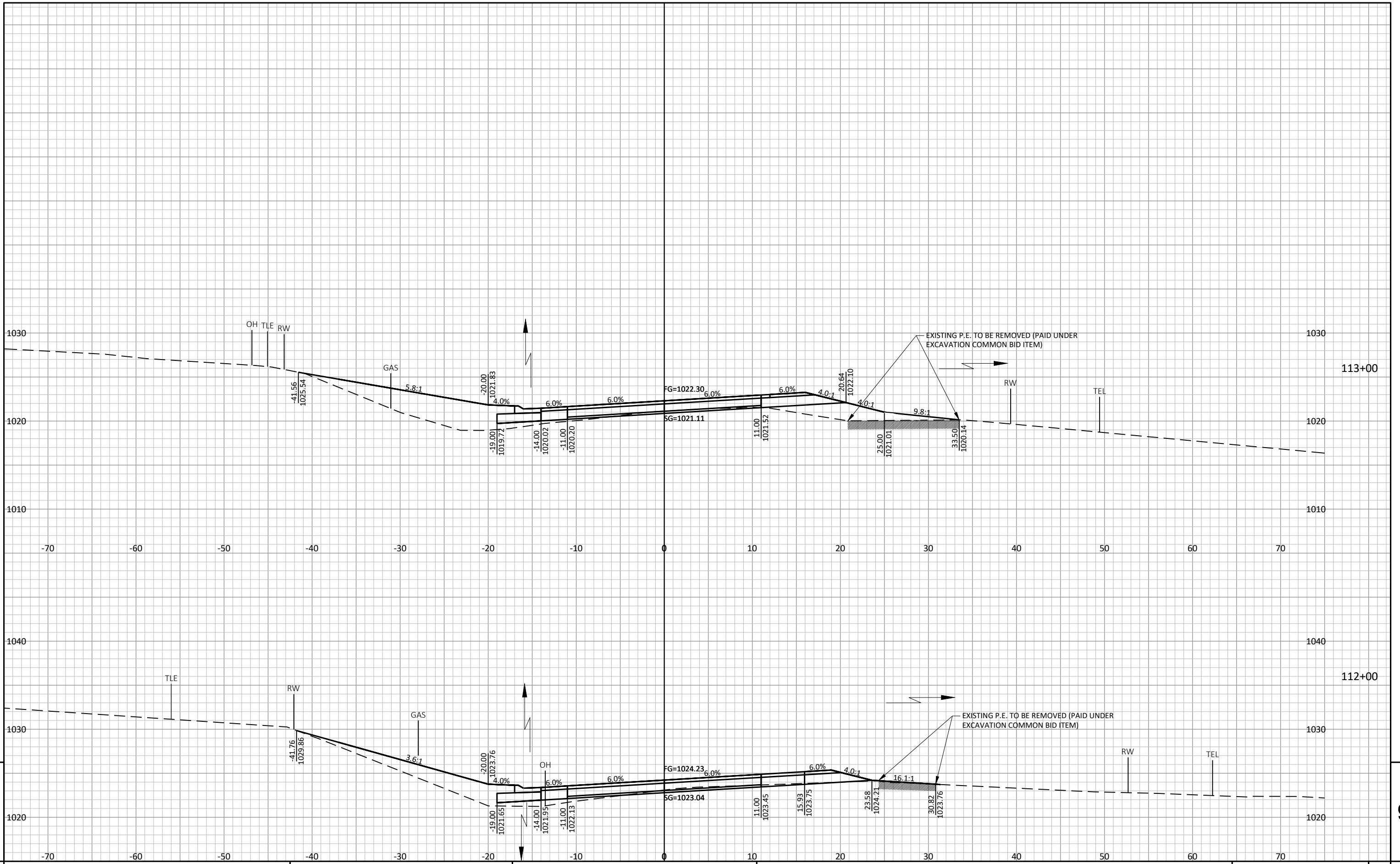
HWY: CTH F

COUNTY: IOWA

CROSS SECTIONS: MAINLINE

SHEET

E



PROJECT NO: 5034-00-72

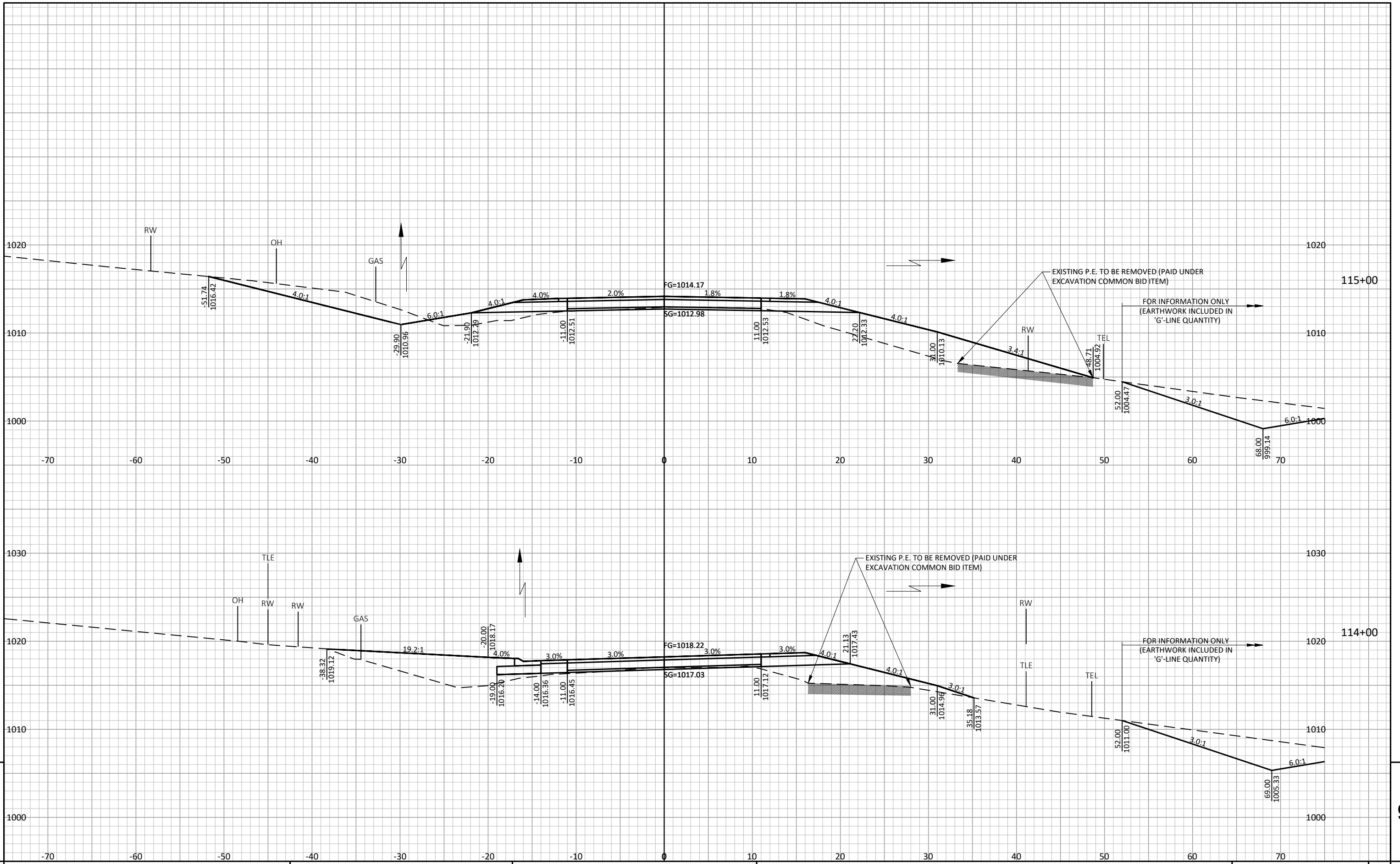
HWY: CTH F

COUNTY: IOWA

CROSS SECTIONS: MAINLINE

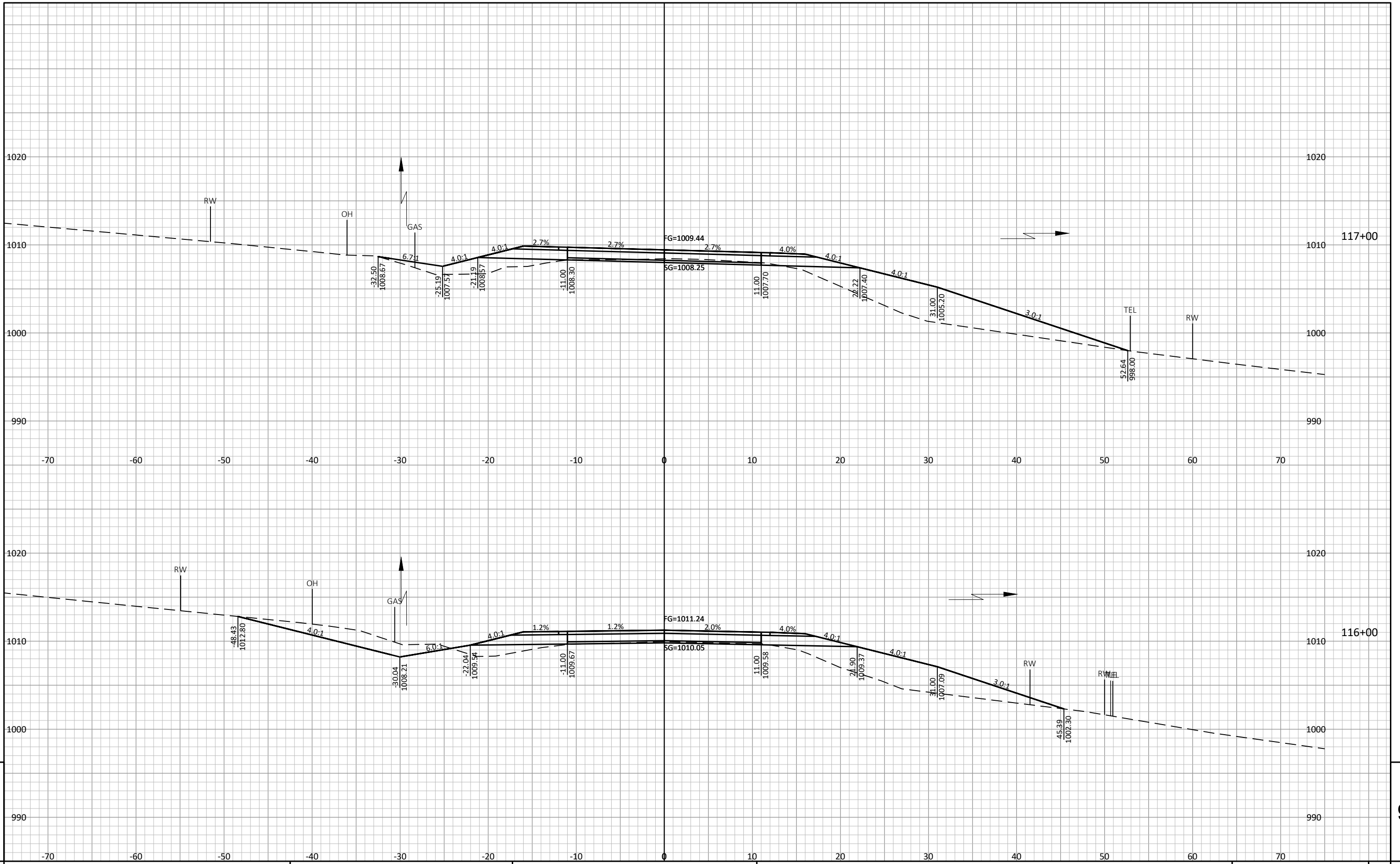
SHEET

E



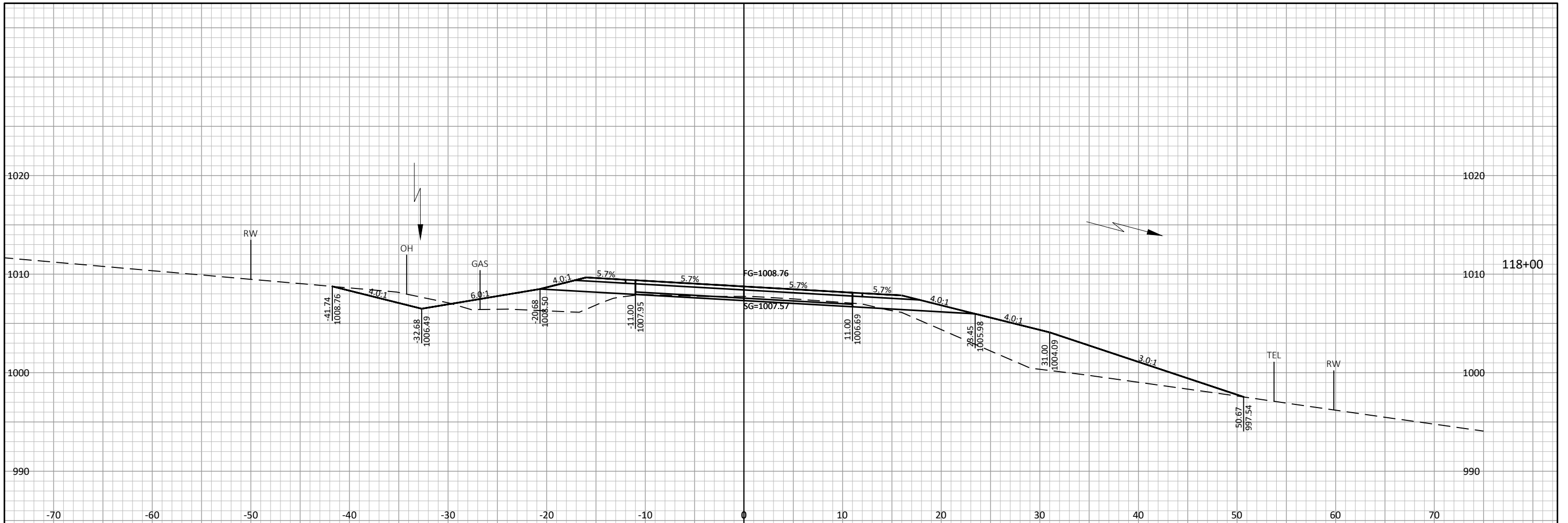
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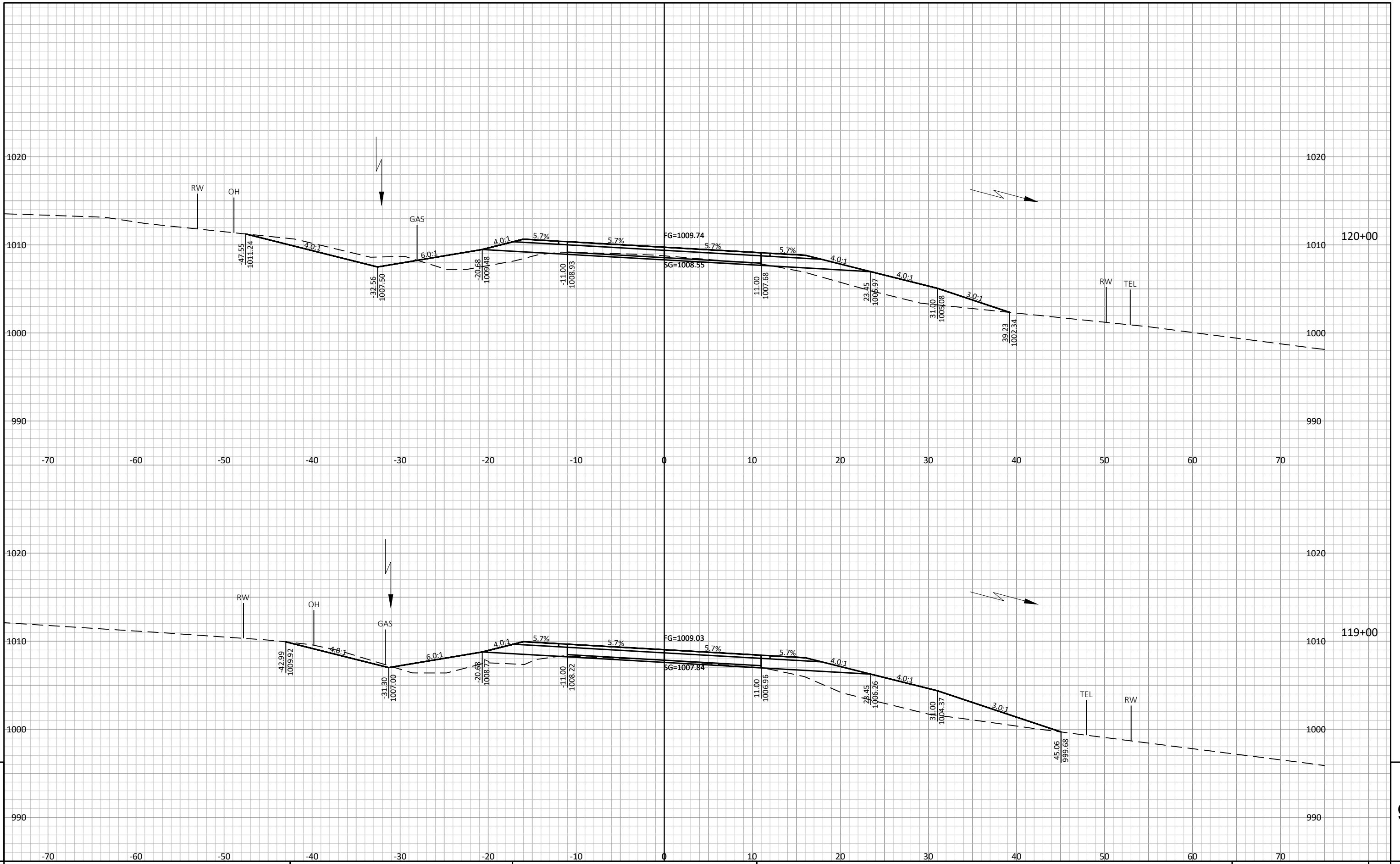
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STATION 117+71
 (1) CULVERT PIPE CORRUGATED STEEL
 24-INCH
 (2) APRON ENDWALLS REQ'D
 SKEW=0°

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PROJECT NO: 5034-00-72

HWY: CTH F

COUNTY: IOWA

CROSS SECTIONS: MAINLINE

SHEET

E

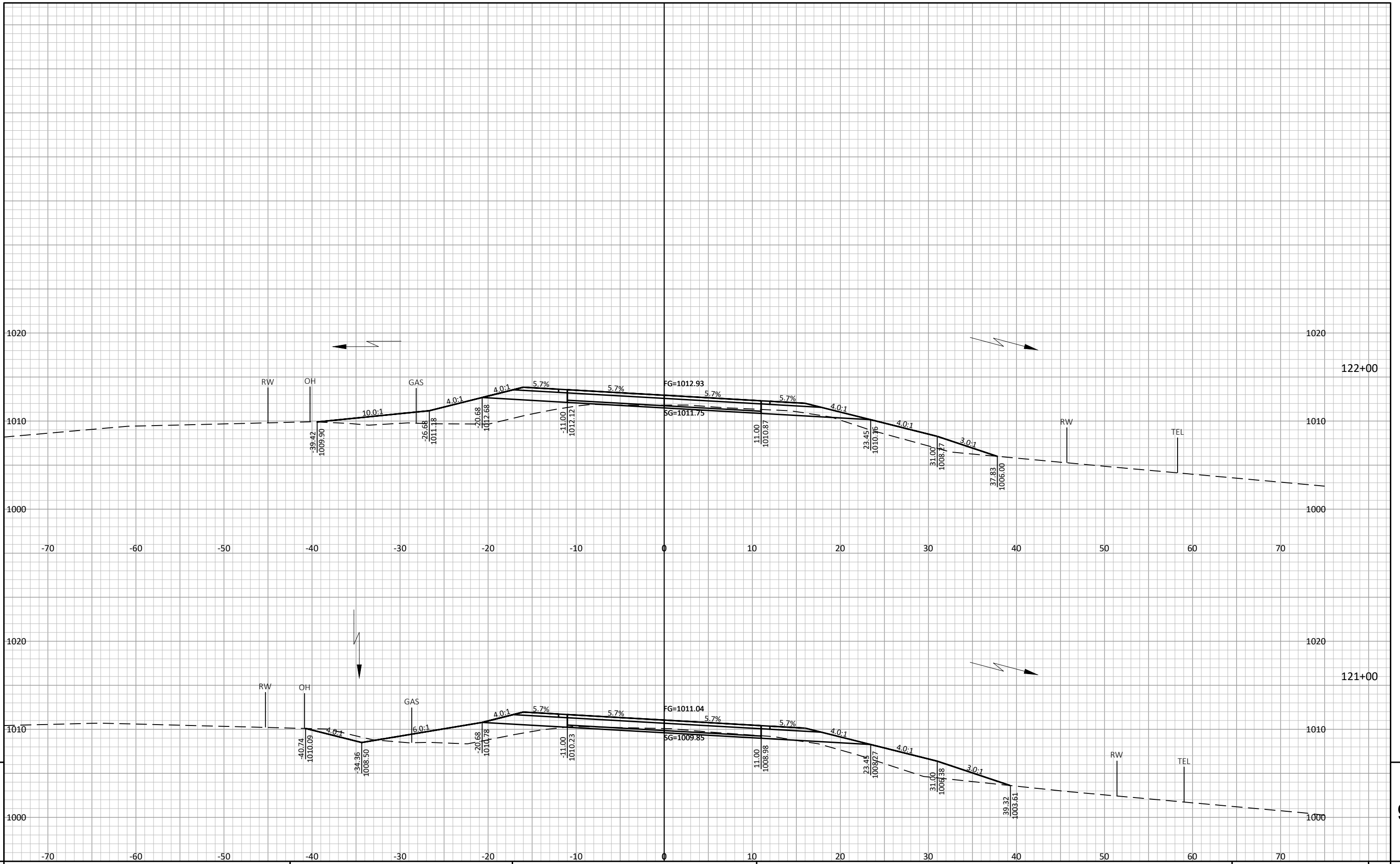
FILE NAME: S:\PROJECTS\K19250 CTH F IOWA COUNTY\DESIGN\CORRIDORS\CORRIDOR_CTH F.DWG

PLOT DATE: 9/8/2021 12:16:00 PM

PLOT BY: JONAH DRAKE

PLOT SCALE: 1" = 1'

LAYOUT: CTH K 52



PROJECT NO: 5034-00-72

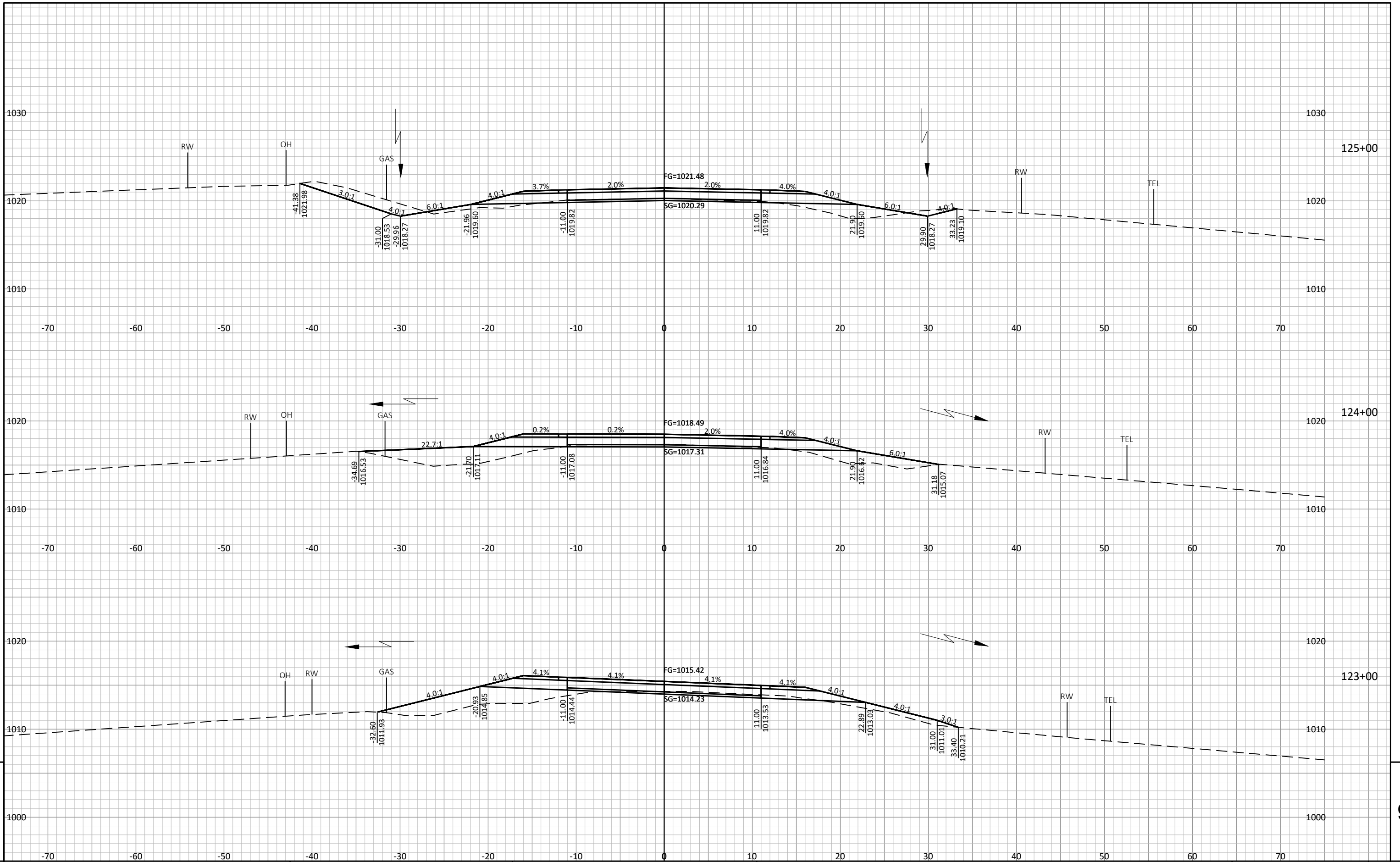
HWY: CTH F

COUNTY: IOWA

CROSS SECTIONS: MAINLINE

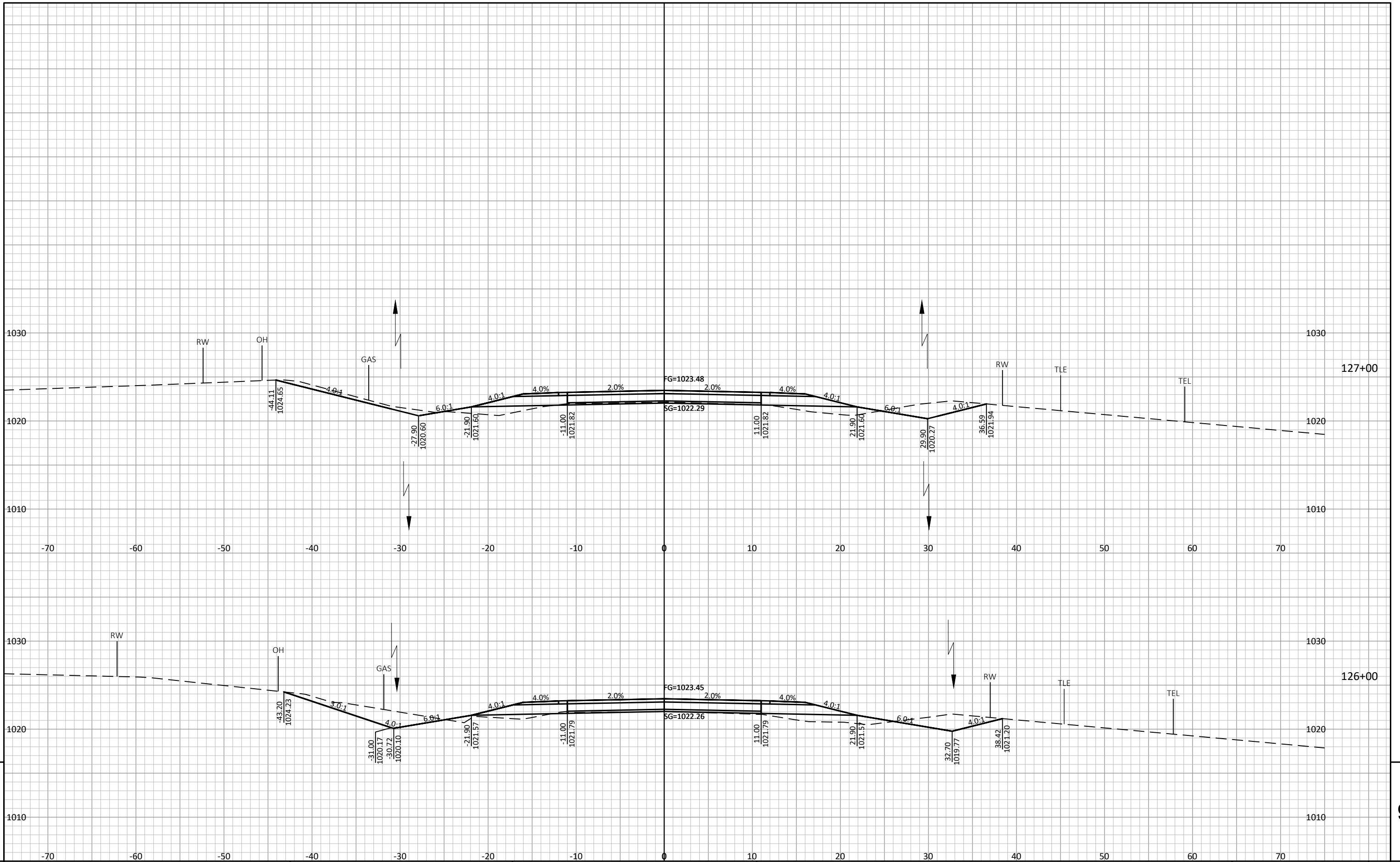
SHEET

E



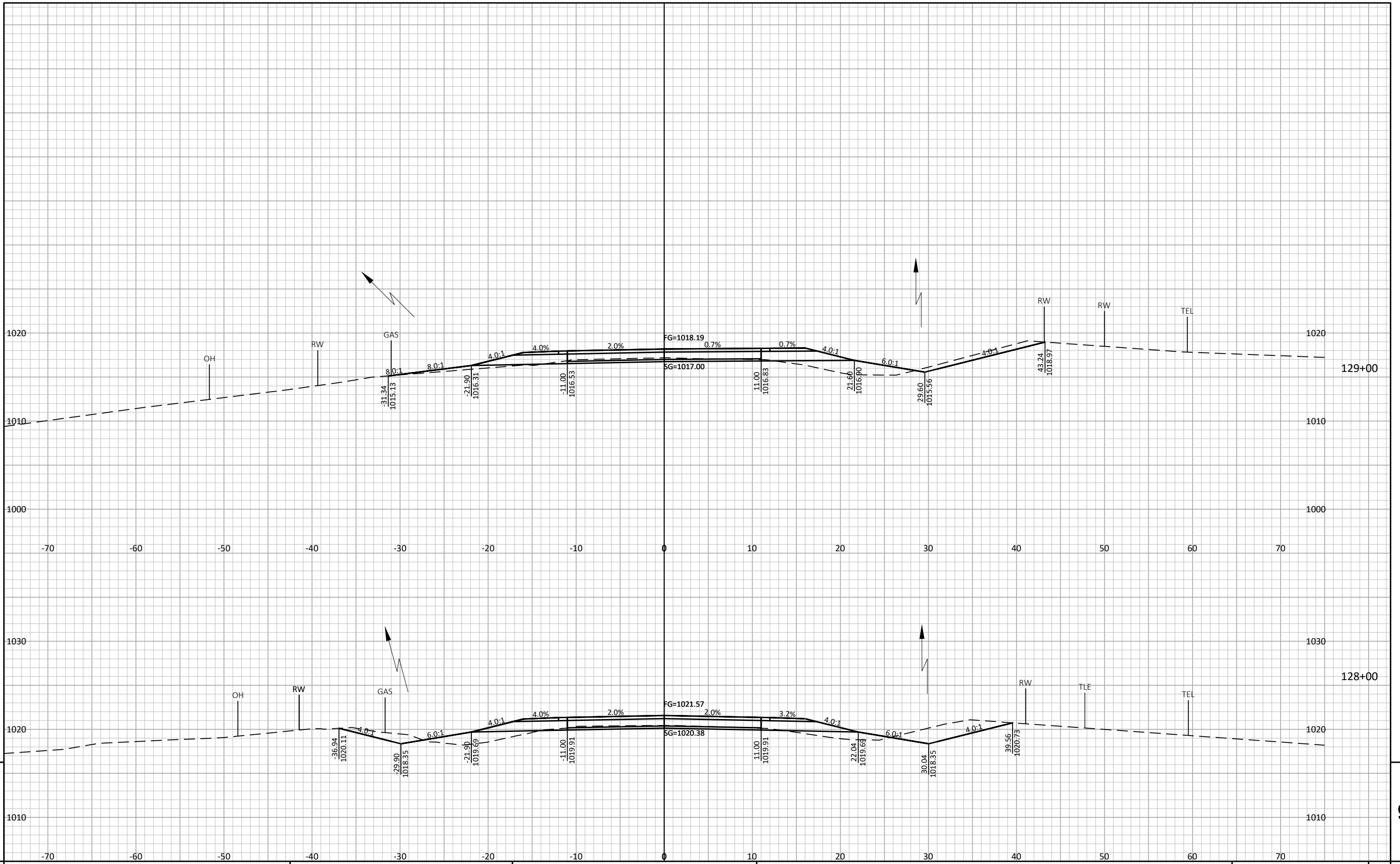
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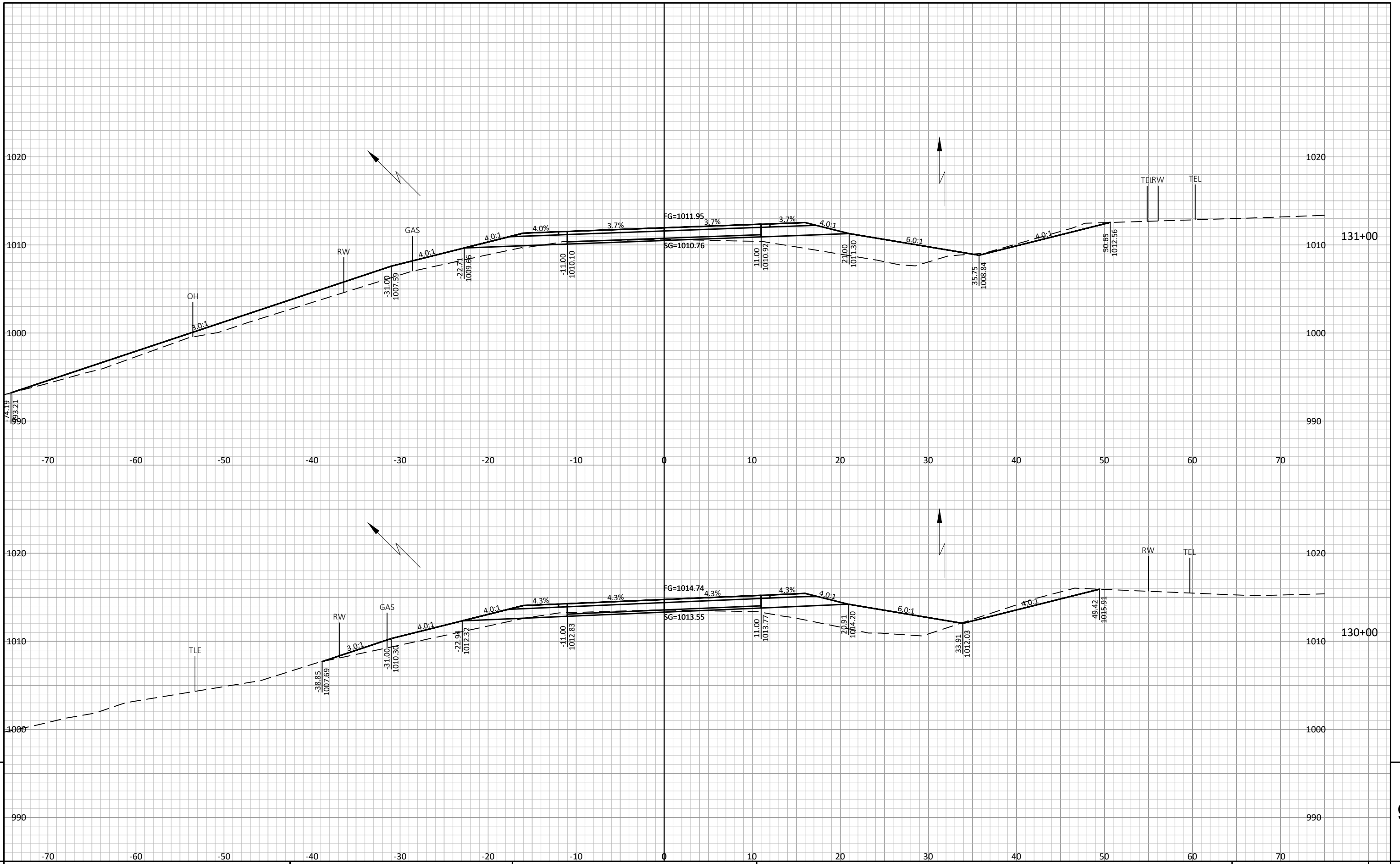
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PROJECT NO: 5034-00-72

HWY: CTH F

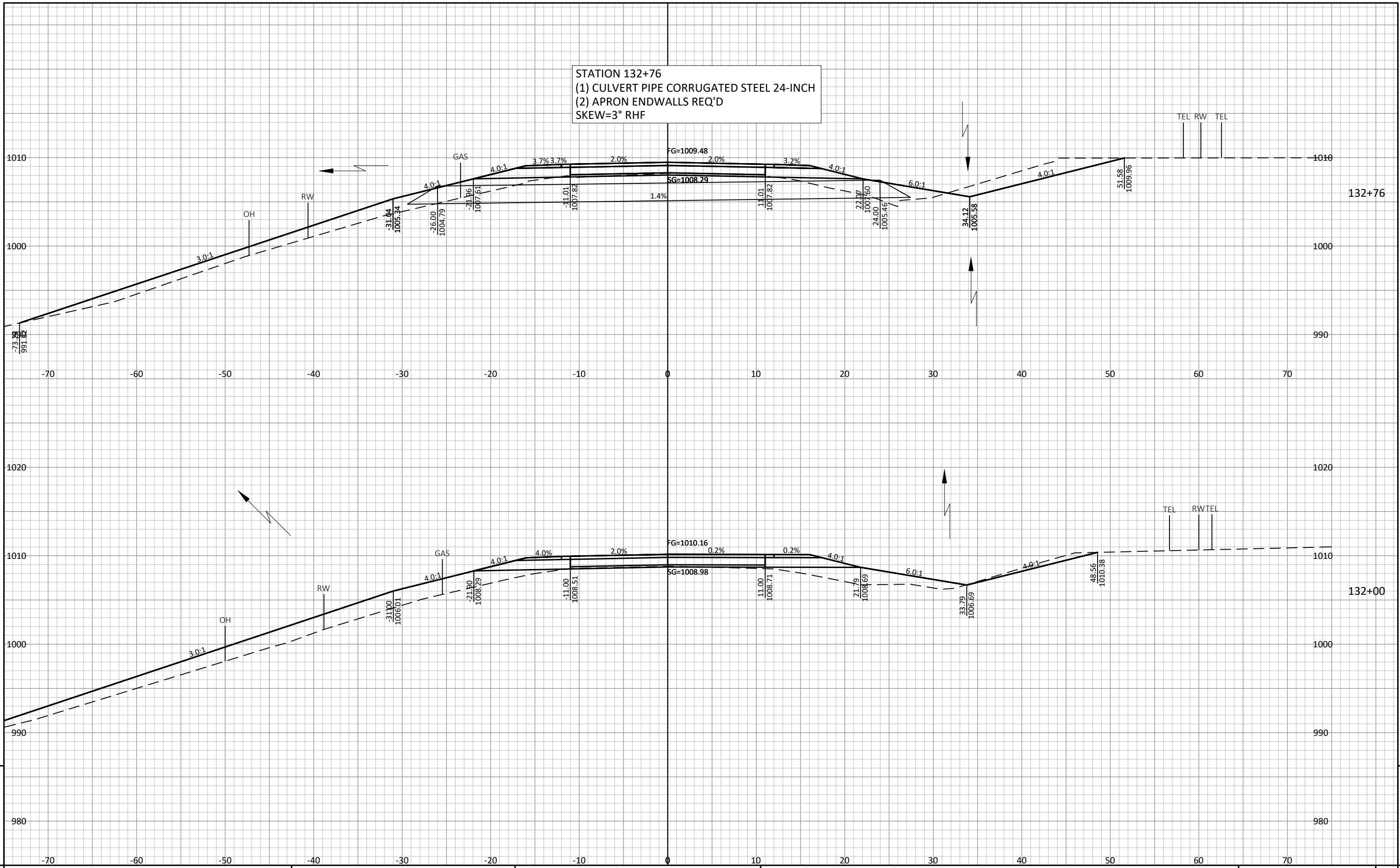
COUNTY: IOWA

CROSS SECTIONS: MAINLINE

SHEET

E

STATION 132+76
 (1) CULVERT PIPE CORRUGATED STEEL 24-INCH
 (2) APRON ENDWALLS REQ'D
 SKEW=3° RHF



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PROJECT NO: 5034-00-72

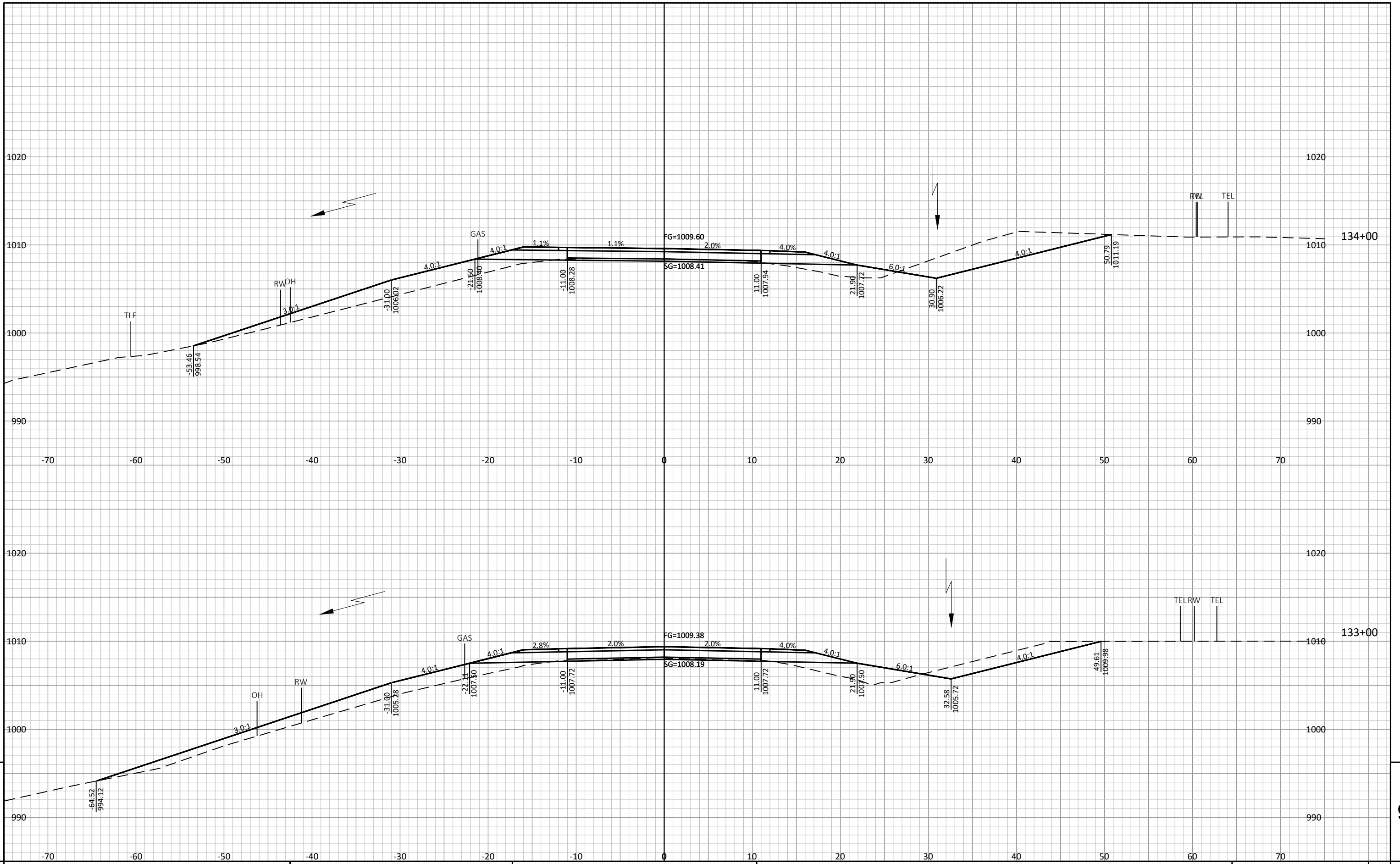
HWY: CTH F

COUNTY: IOWA

CROSS SECTIONS: MAINLINE

SHEET

E



PROJECT NO: 5034-00-72

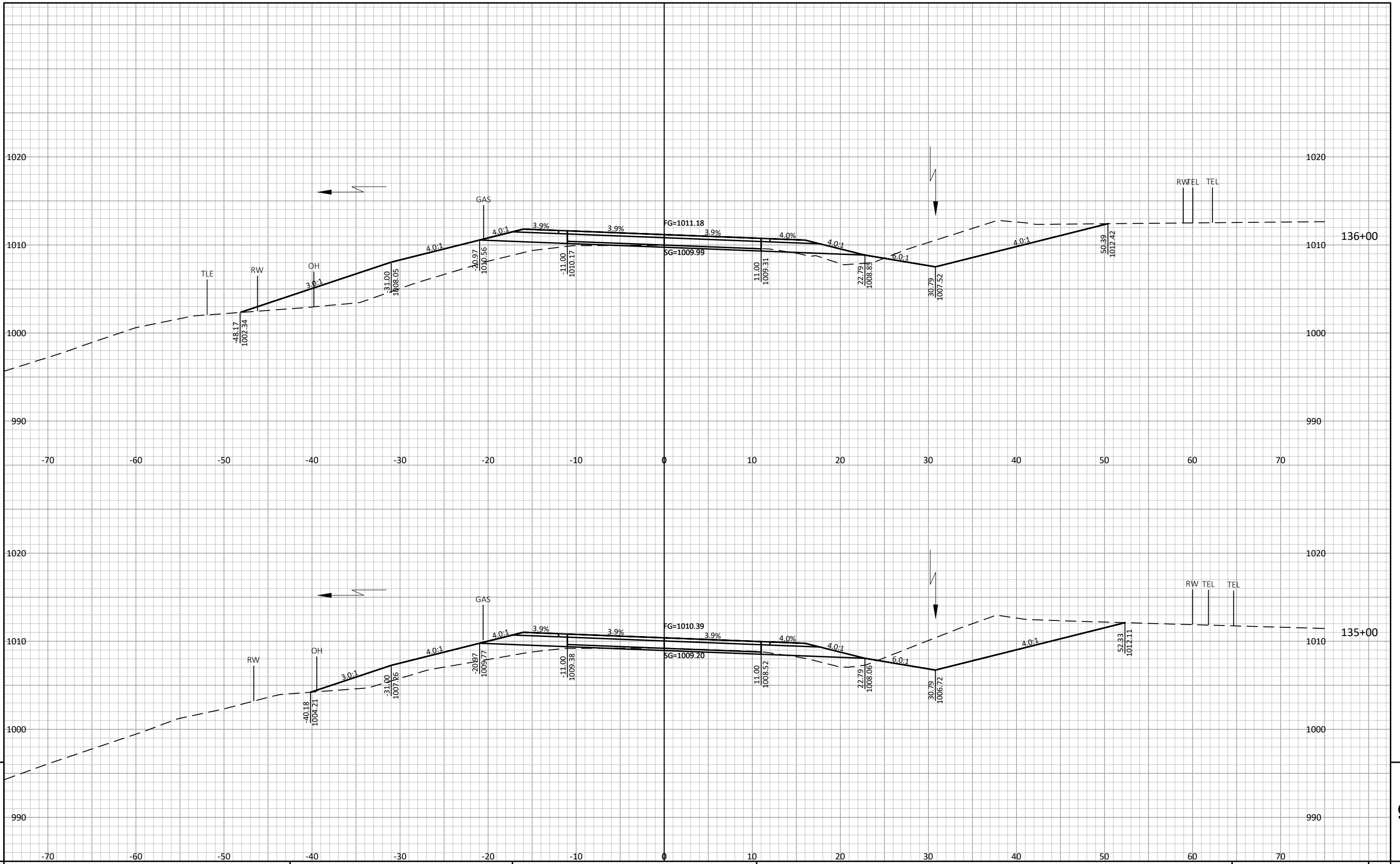
HWY: CTH F

COUNTY: IOWA

CROSS SECTIONS: MAINLINE

SHEET

E



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PROJECT NO: 5034-00-72

HWY: CTH F

COUNTY: IOWA

CROSS SECTIONS: MAINLINE

SHEET

E

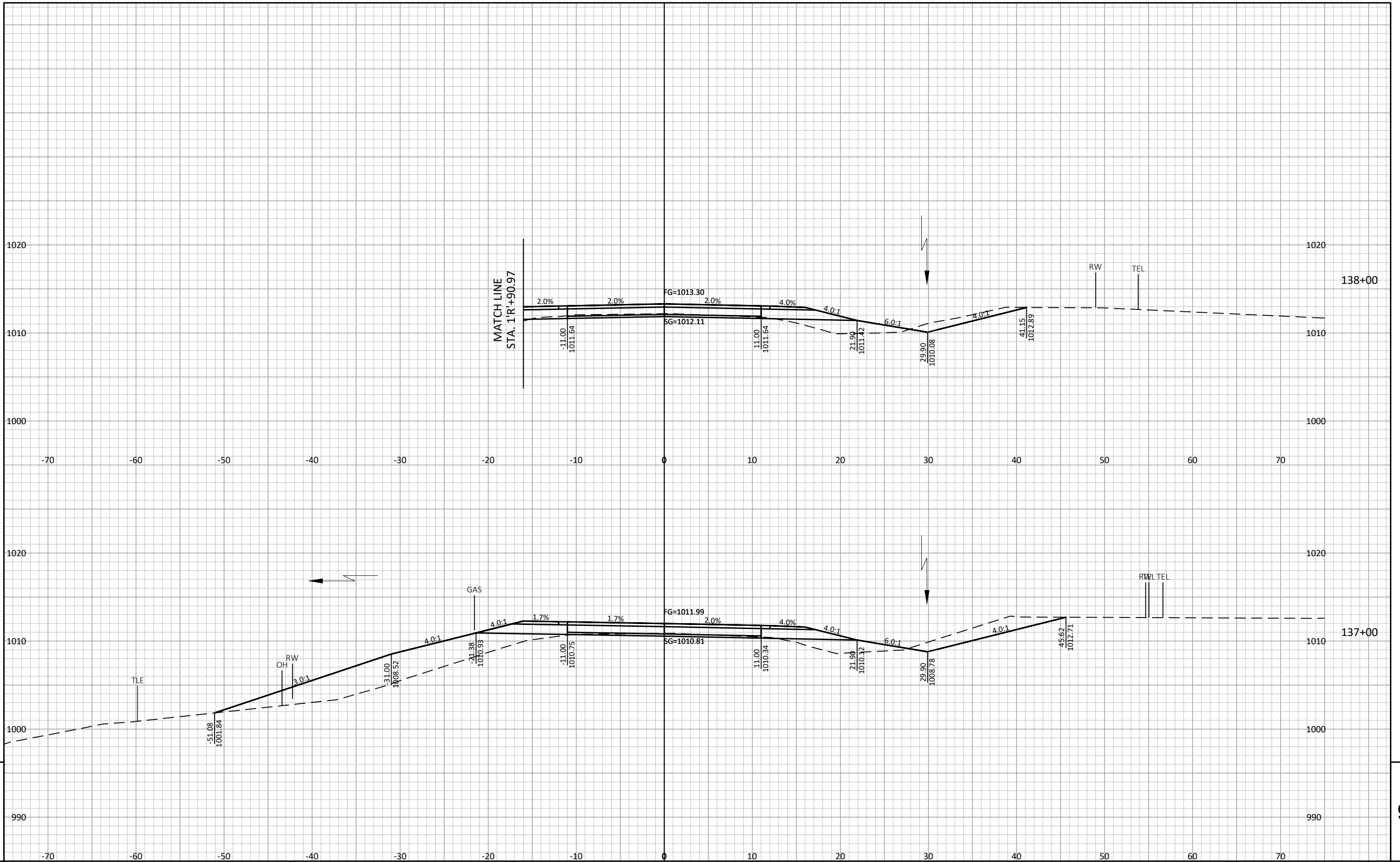
FILE NAME : S:\PROJECTS\K19250 CTH F IOWA COUNTY\DESIGN\CORRIDORS\CORRIDOR_CTH F.DWG

PLOT DATE : 9/8/2021 12:17:40 PM

PLOT BY : JONAH DRAKE

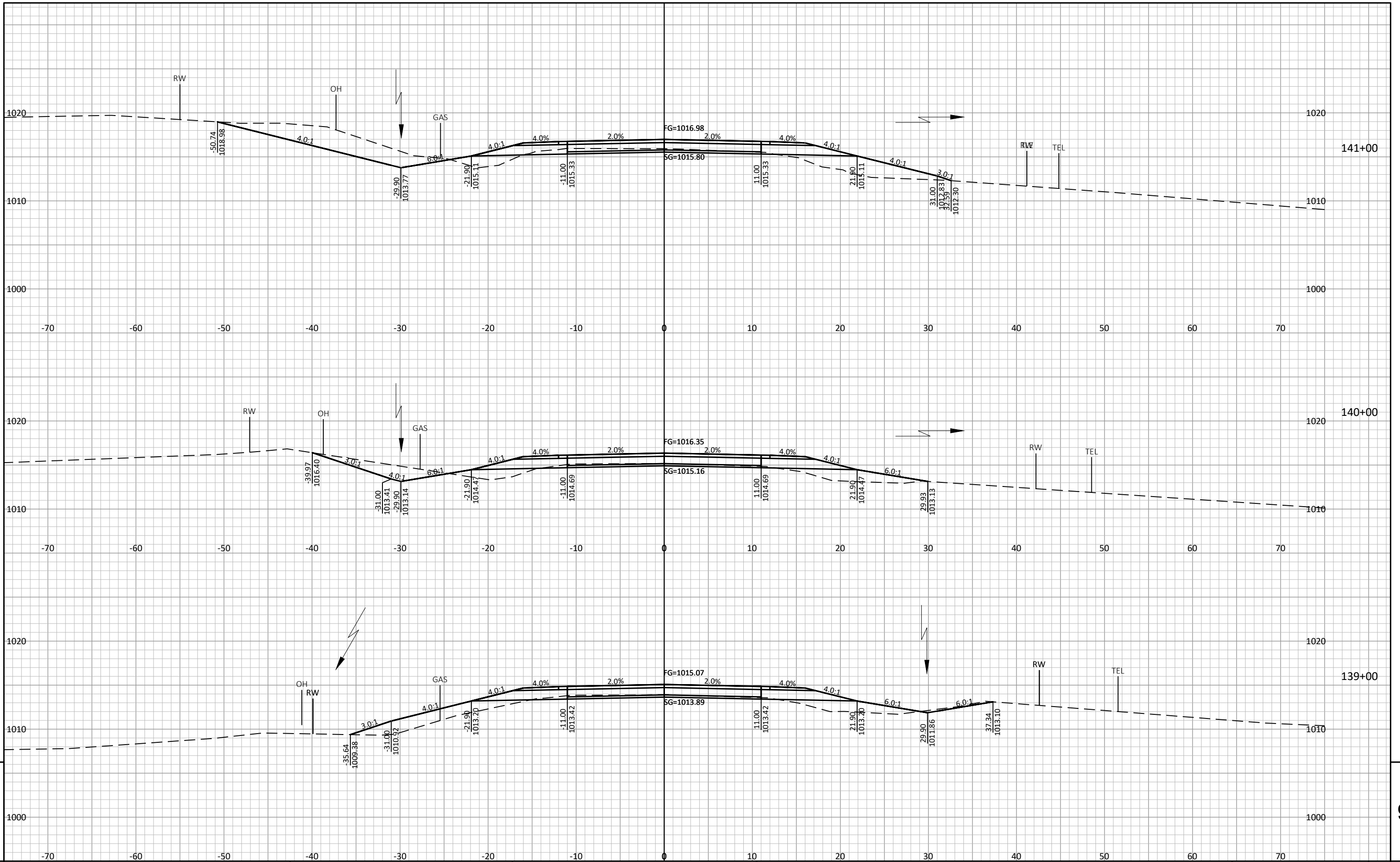
PLOT SCALE : 1" = 1'

LAYOUT : CTH K 60



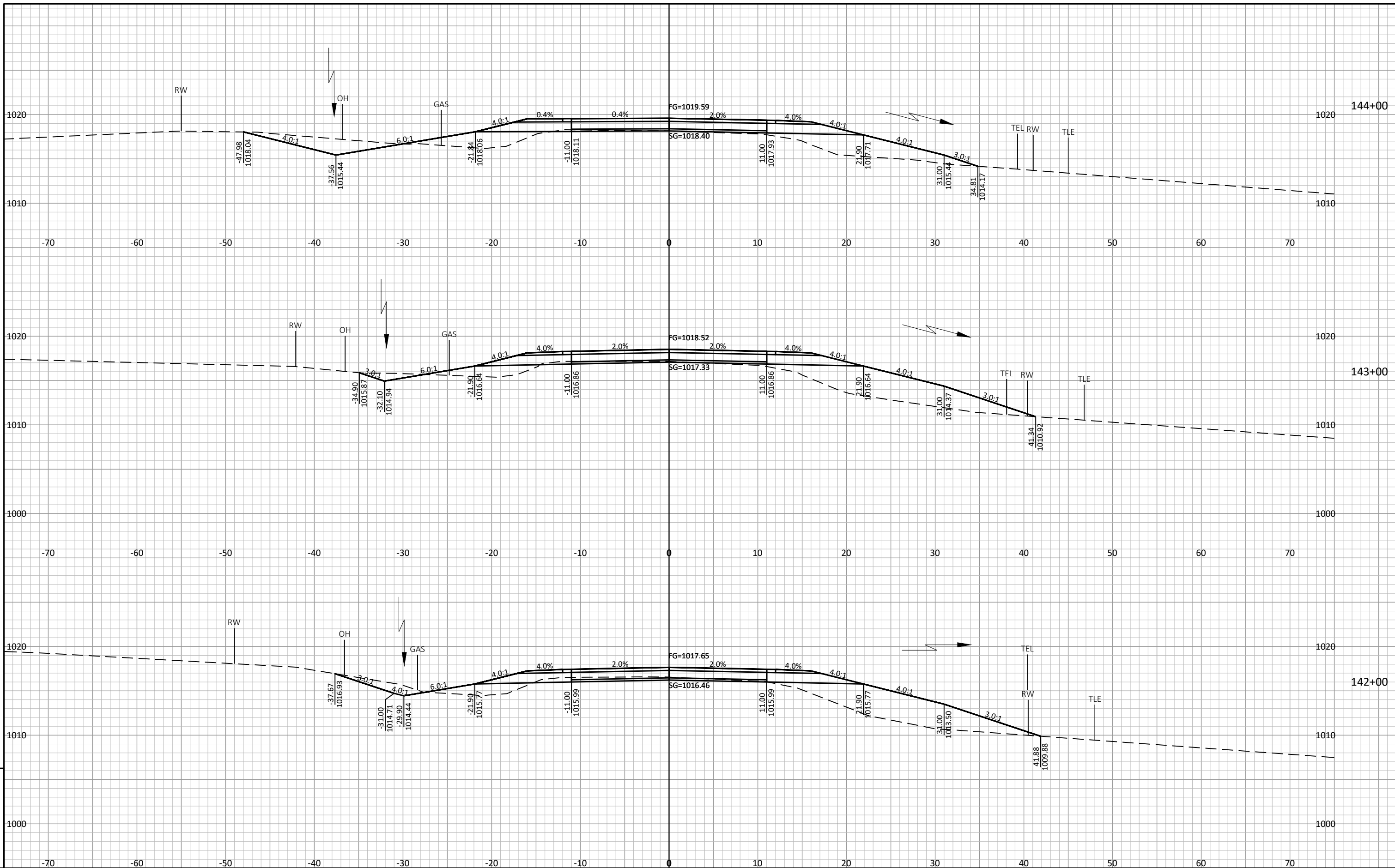
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PROJECT NO: 5034-00-72

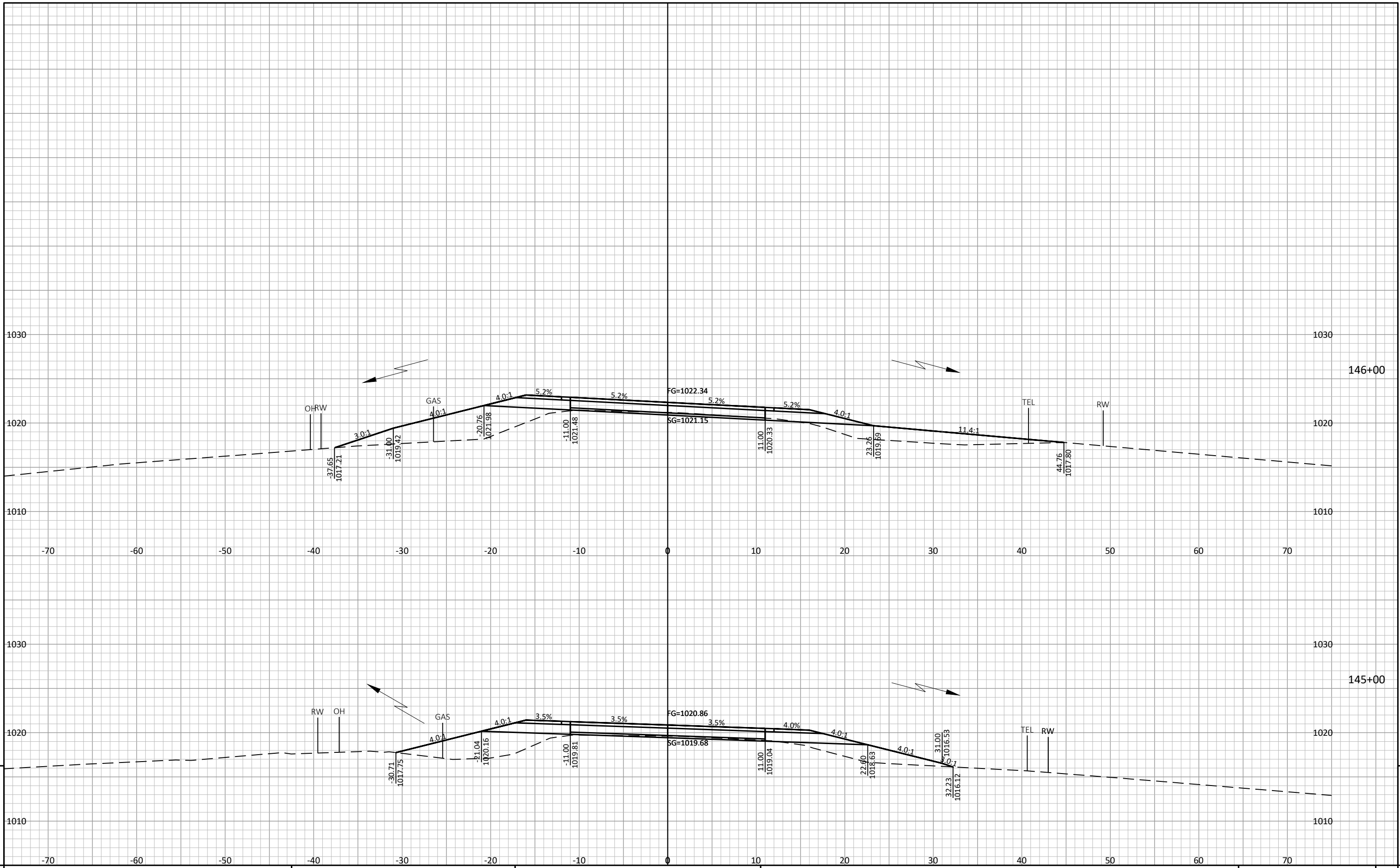
HWY: CTH F

COUNTY: IOWA

CROSS SECTIONS: MAINLINE

SHEET

E



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PROJECT NO: 5034-00-72

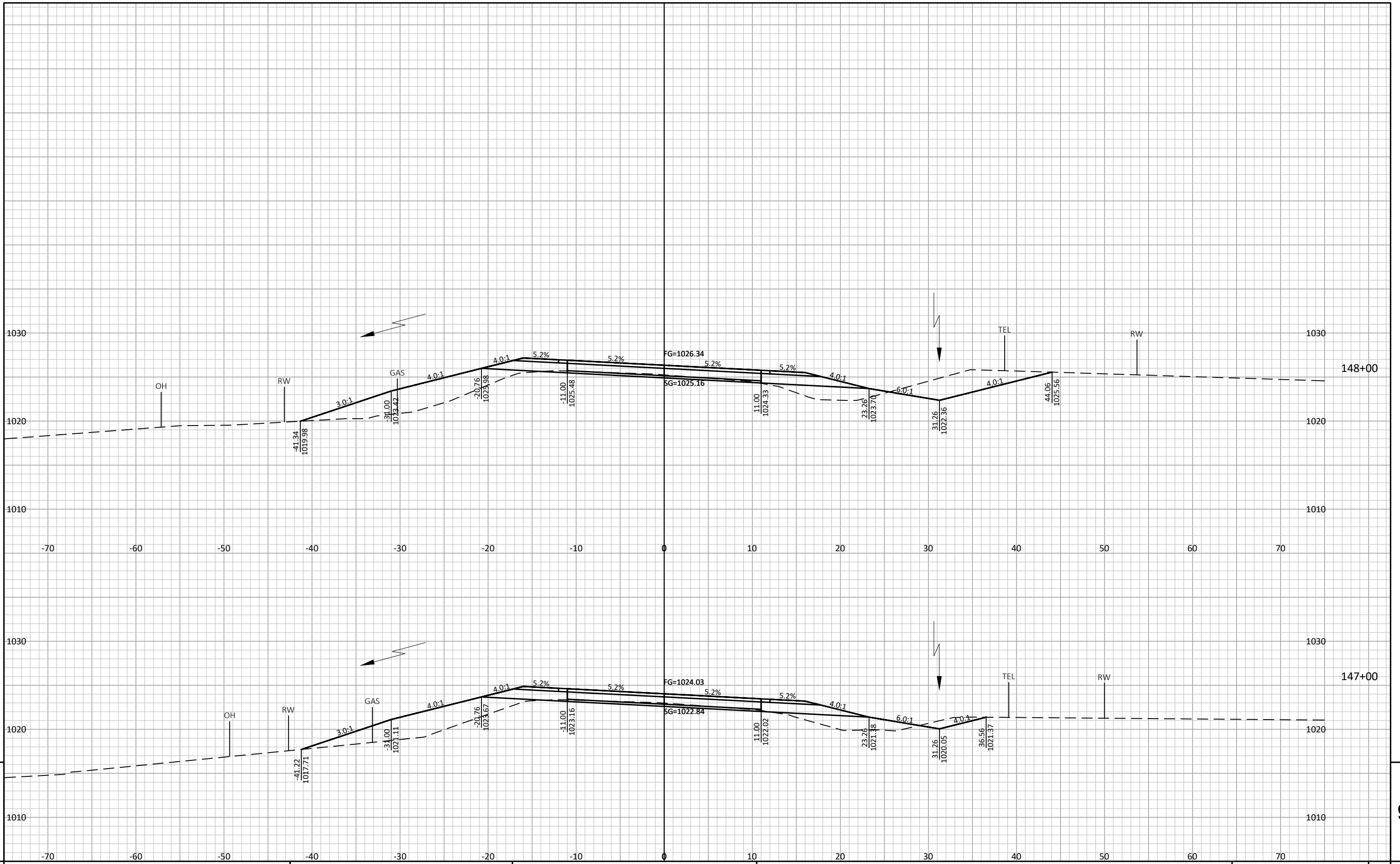
HWY: CTH F

COUNTY: IOWA

CROSS SECTIONS: MAINLINE

SHEET

E



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PROJECT NO: 5034-00-72

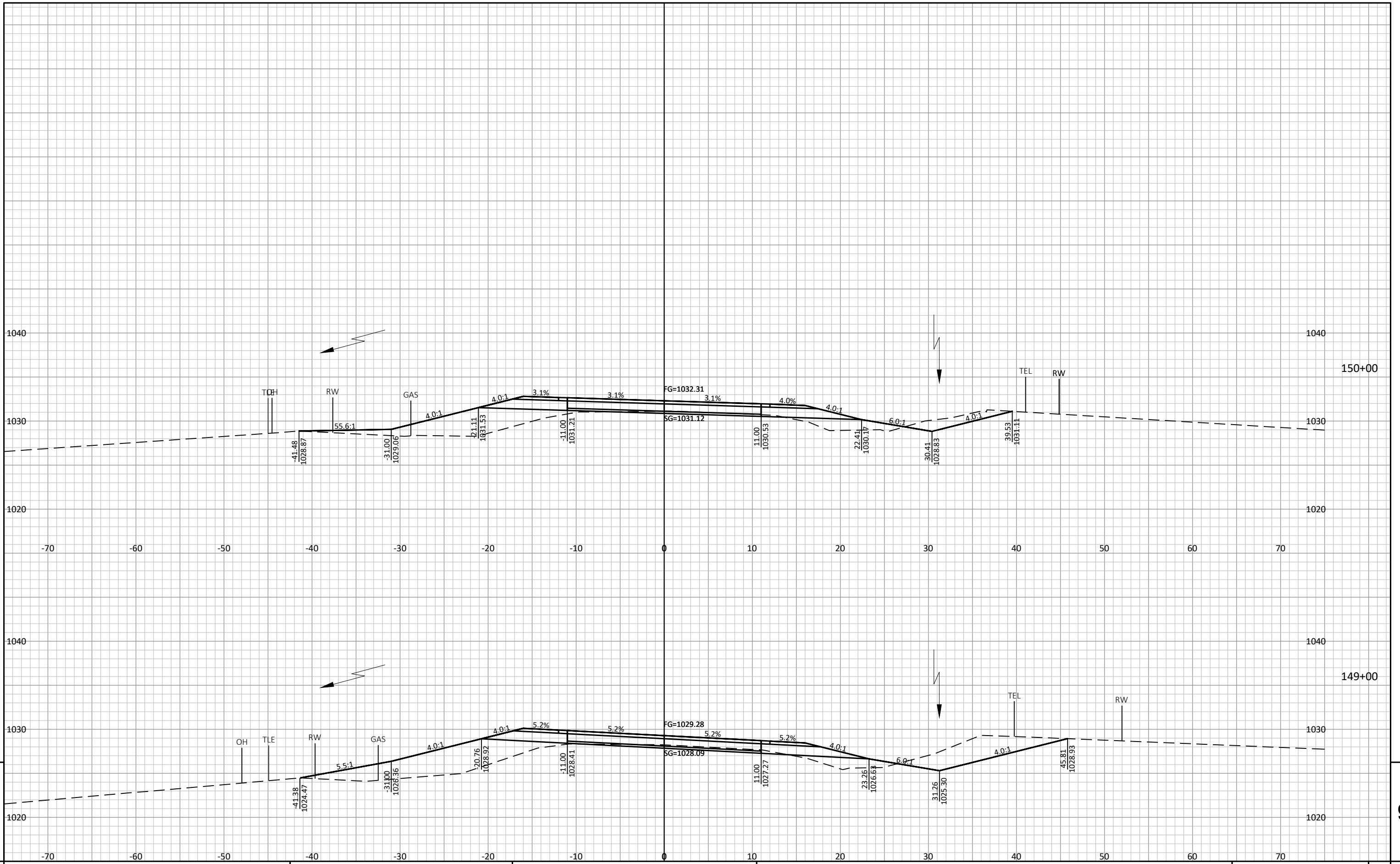
HWY: CTH F

COUNTY: IOWA

CROSS SECTIONS: MAINLINE

SHEET

E



PROJECT NO: 5034-00-72

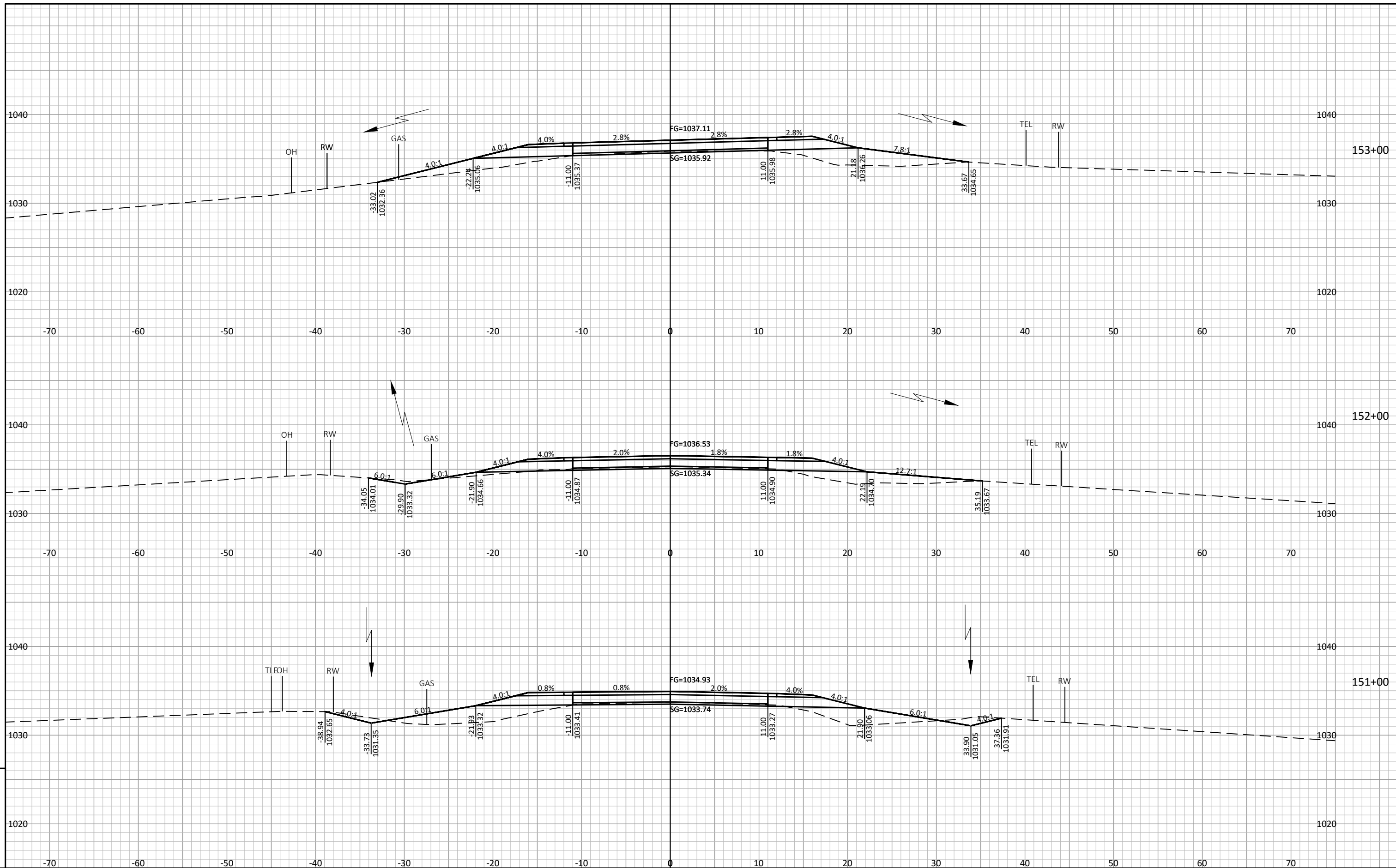
HWY: CTH F

COUNTY: IOWA

CROSS SECTIONS: MAINLINE

SHEET

E



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PROJECT NO: 5034-00-72

HWY: CTH F

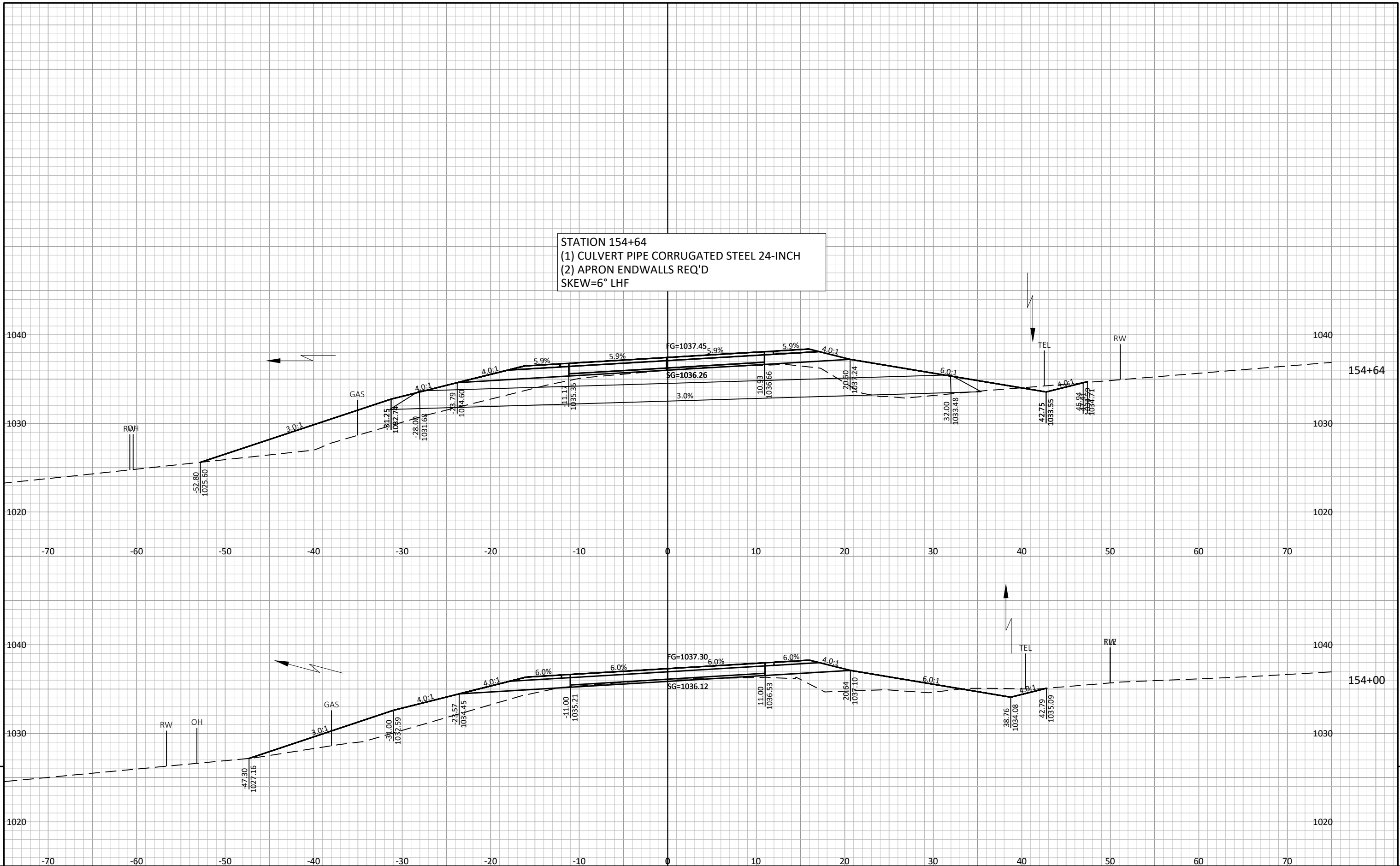
COUNTY: IOWA

CROSS SECTIONS: MAINLINE

SHEET

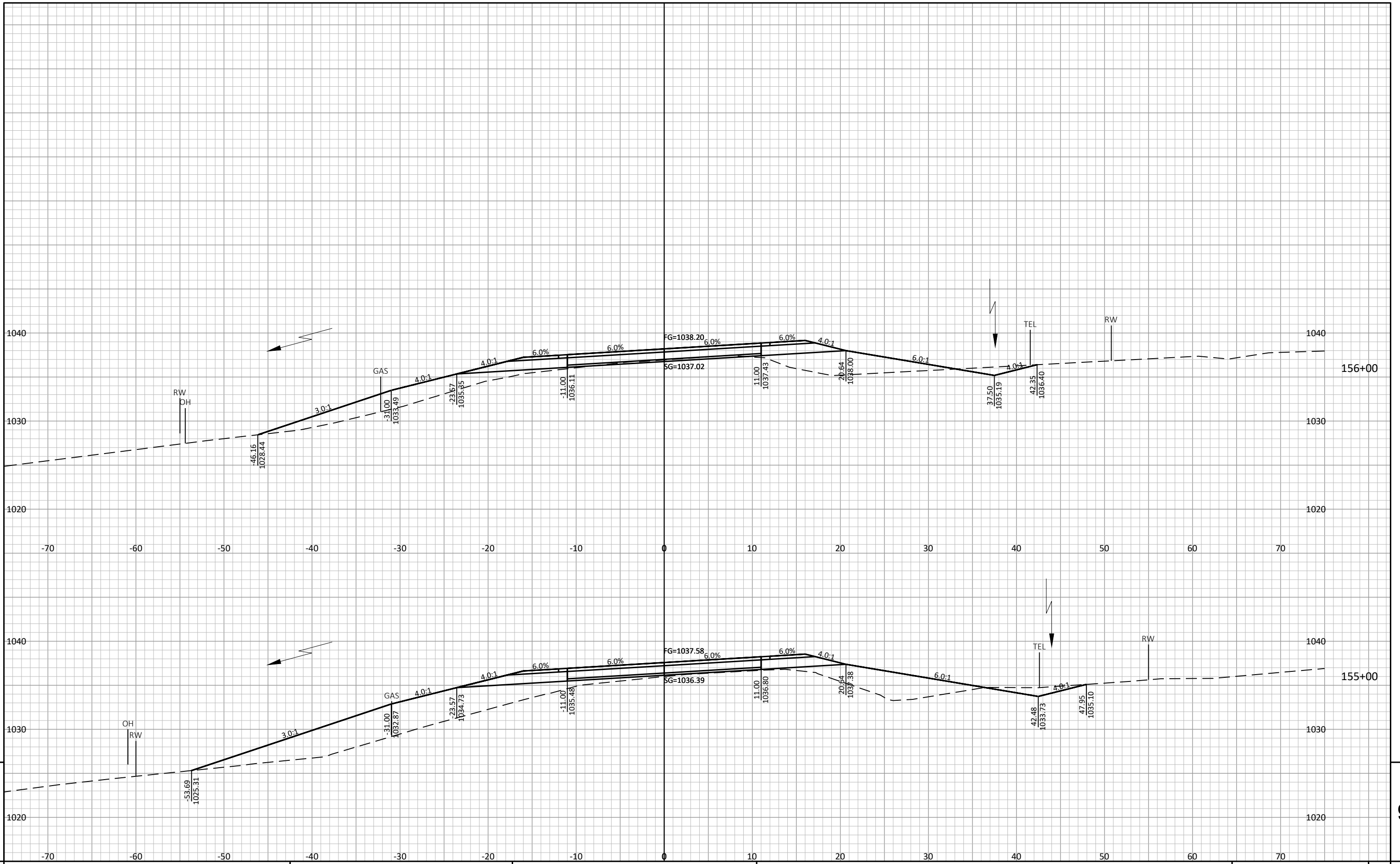
E

STATION 154+64
 (1) CULVERT PIPE CORRUGATED STEEL 24-INCH
 (2) APRON ENDWALLS REQ'D
 SKEW=6° LHF



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PROJECT NO: 5034-00-72

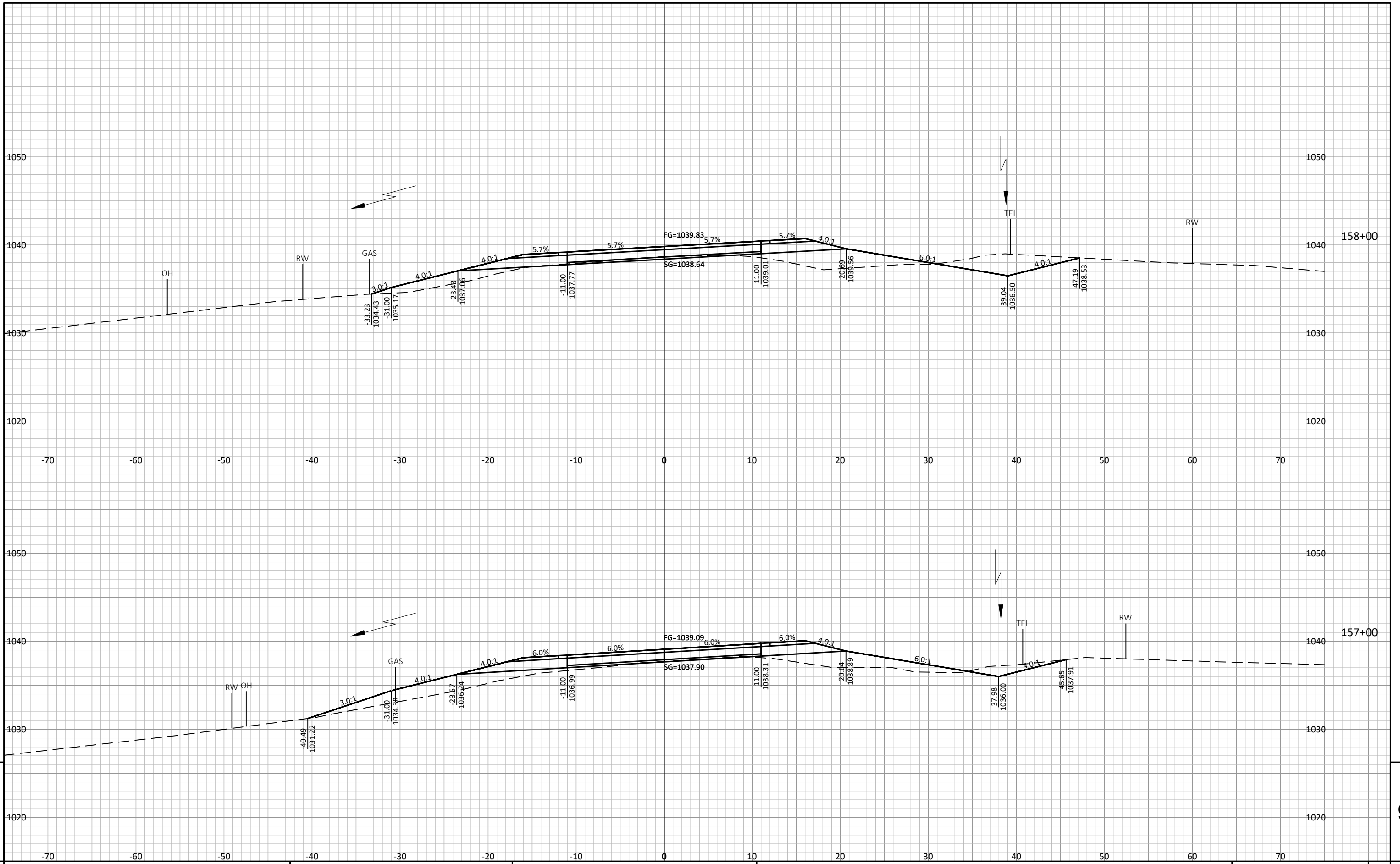
HWY: CTH F

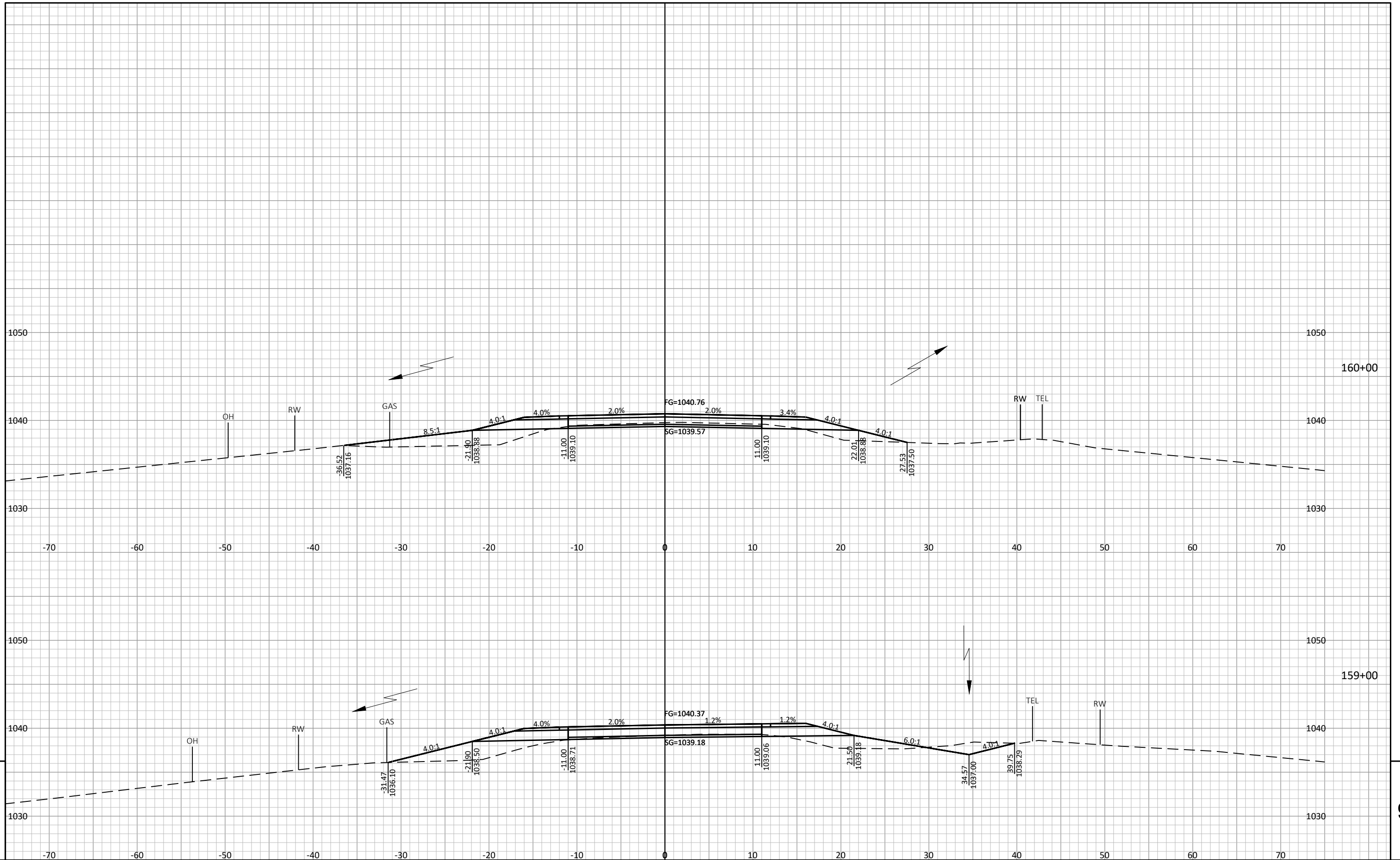
COUNTY: IOWA

CROSS SECTIONS: MAINLINE

SHEET

E





PROJECT NO: 5034-00-72

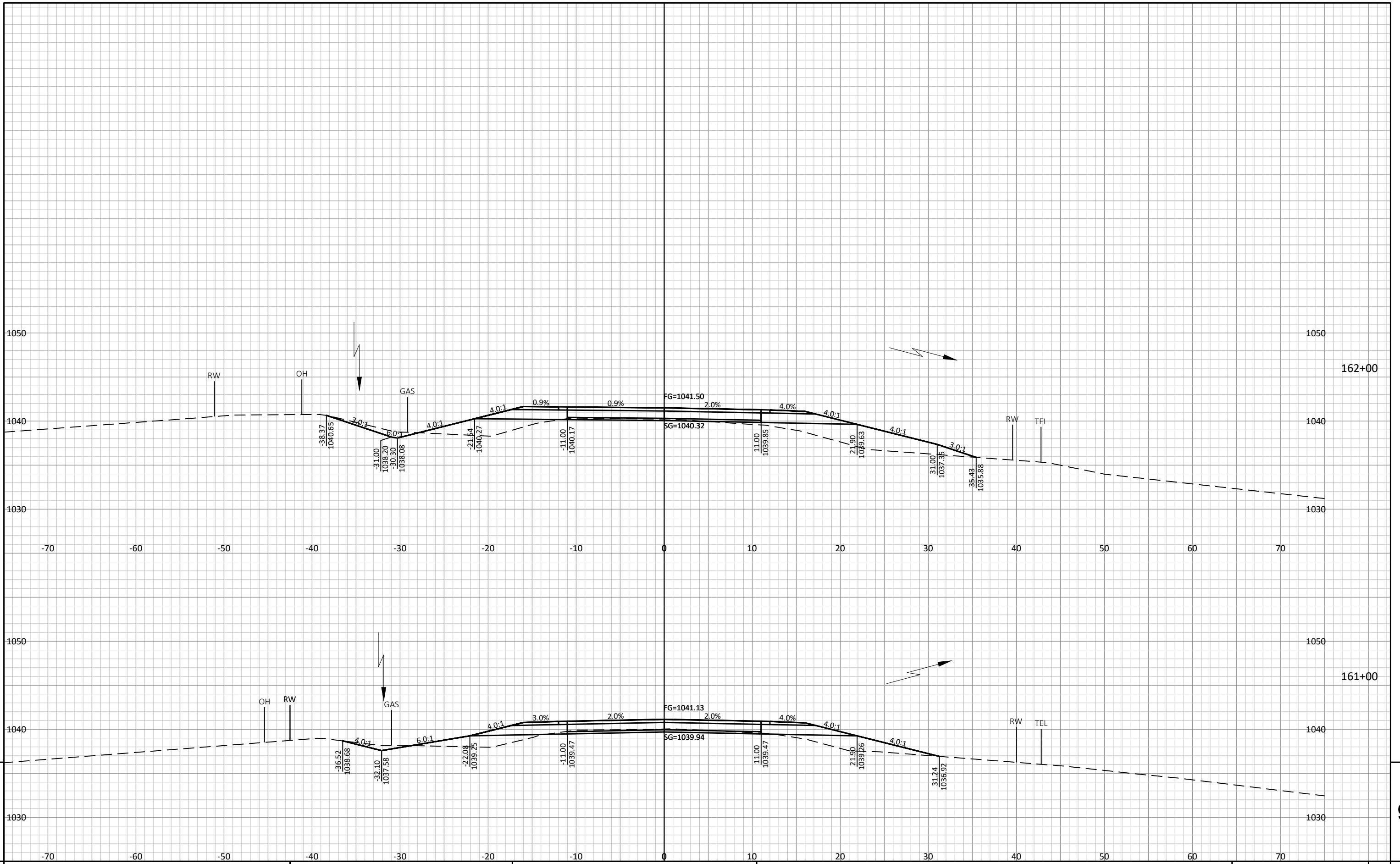
HWY: CTH F

COUNTY: IOWA

CROSS SECTIONS: MAINLINE

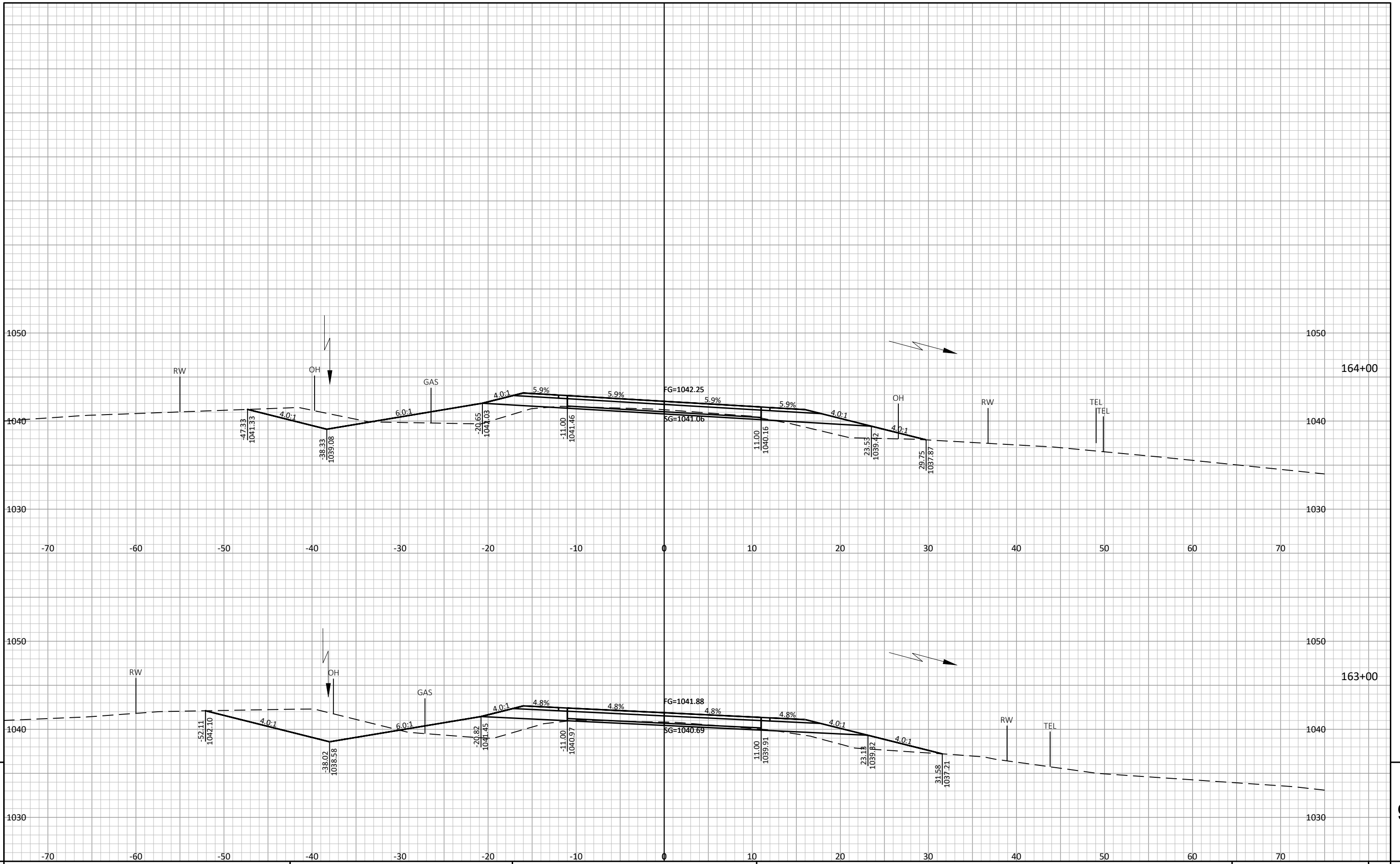
SHEET

E



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PROJECT NO: 5034-00-72

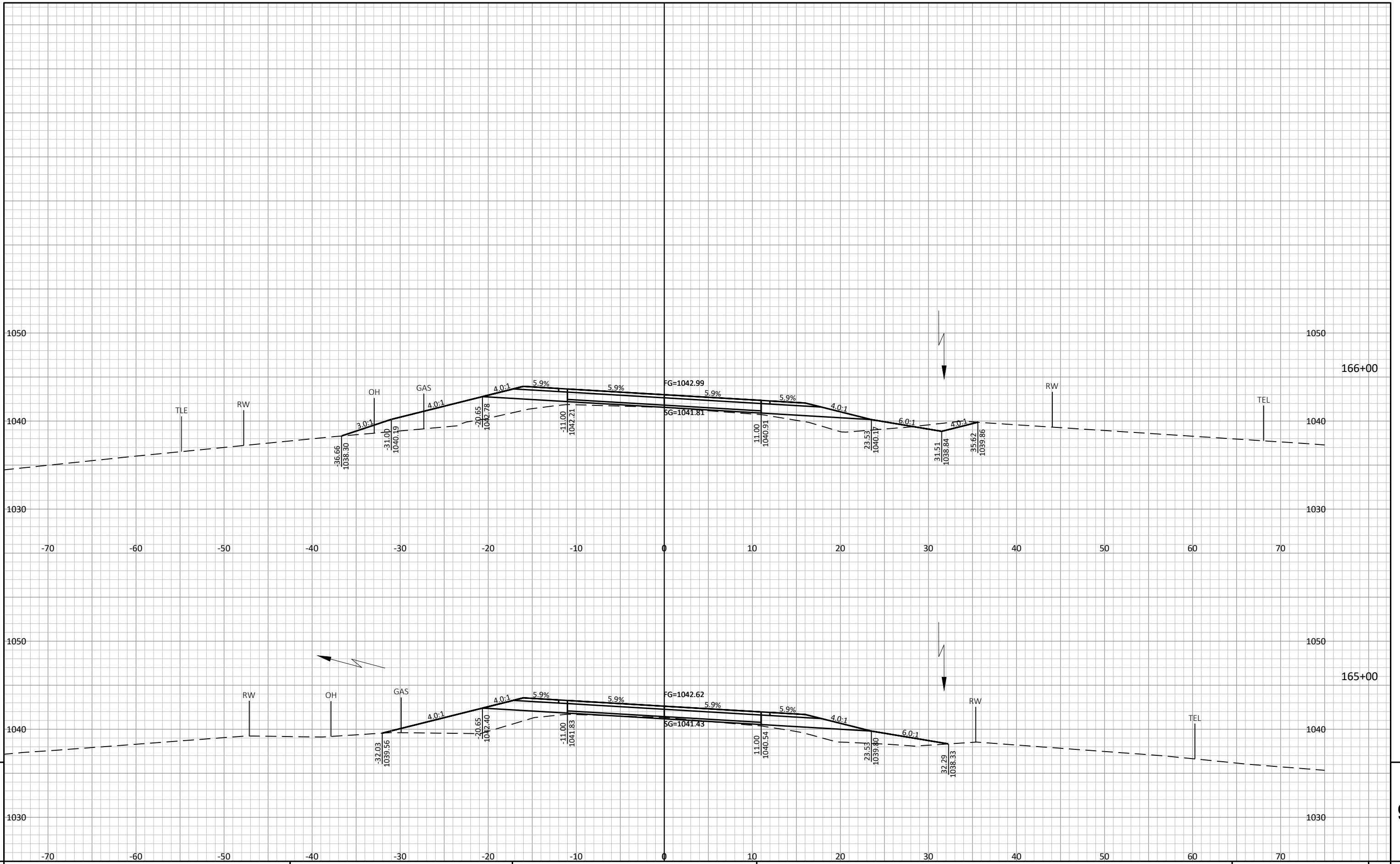
HWY: CTH F

COUNTY: IOWA

CROSS SECTIONS: MAINLINE

SHEET

E



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PROJECT NO: 5034-00-72

HWY: CTH F

COUNTY: IOWA

CROSS SECTIONS: MAINLINE

SHEET

E

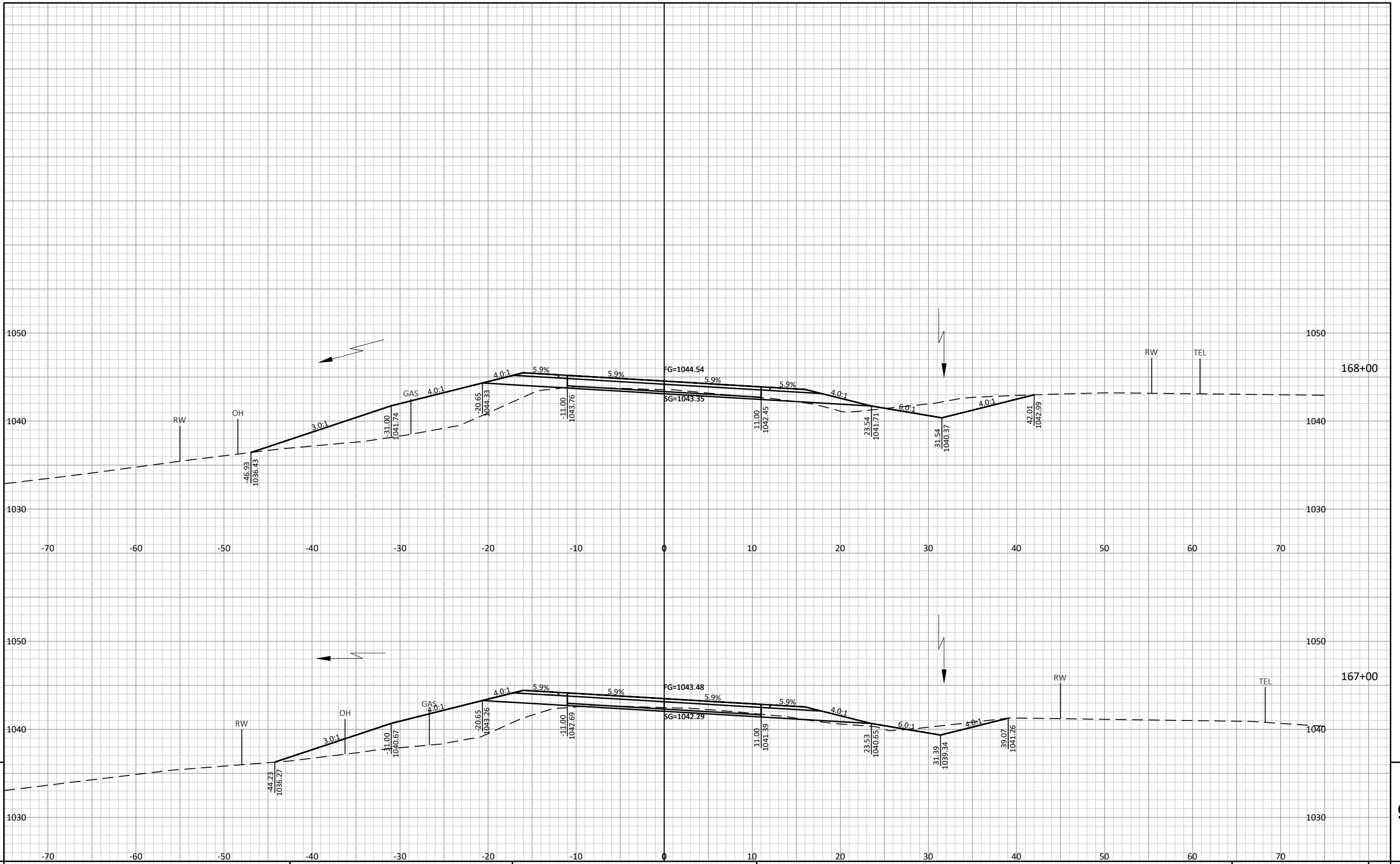
FILE NAME : S:\PROJECTS\K19250 CTH F IOWA COUNTY\DESIGN\CORRIDORS\CORRIDOR_CTH F.DWG

PLOT DATE : 9/8/2021 12:20:32 PM

PLOT BY : JONAH DRAKE

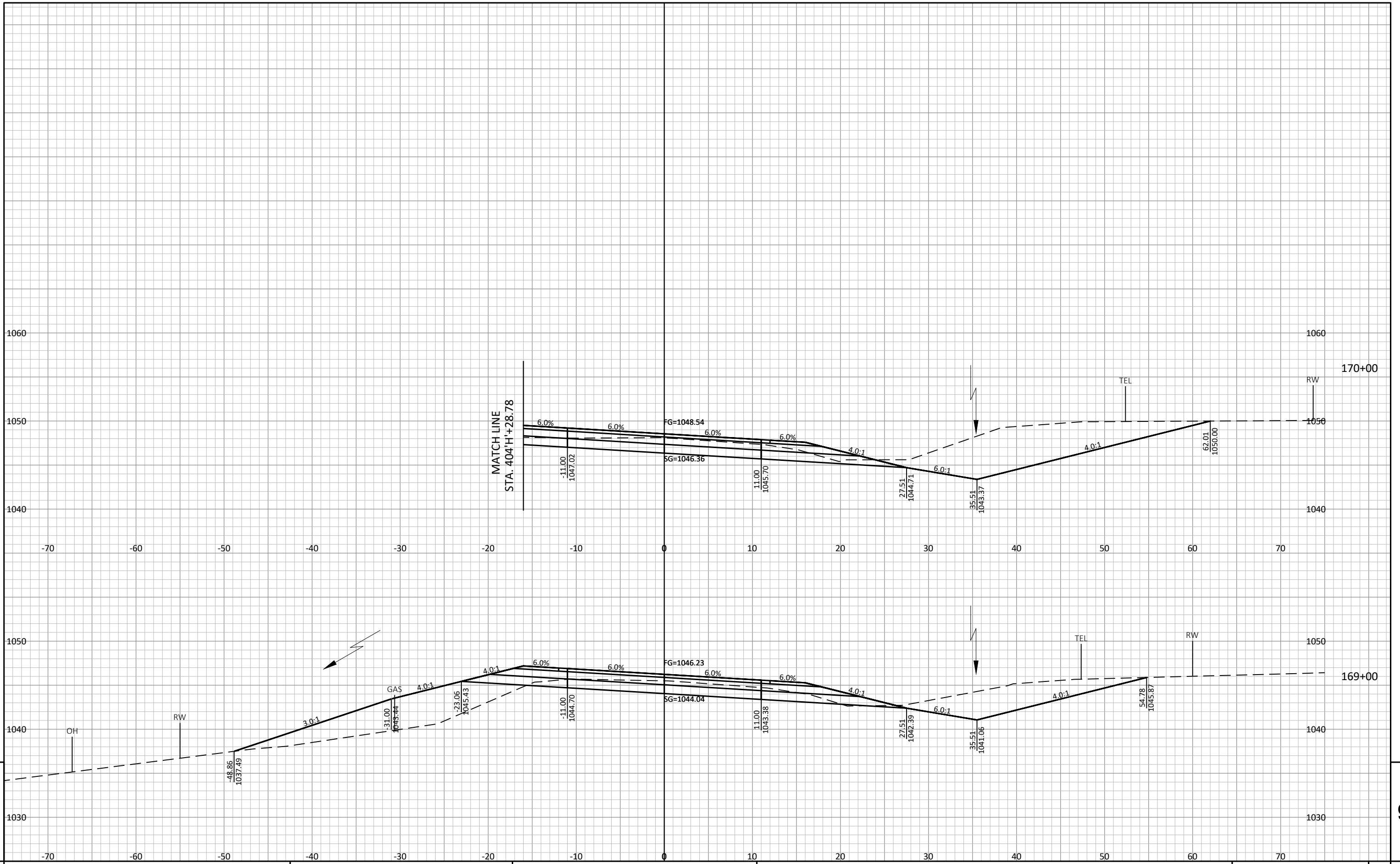
PLOT SCALE : 1" = 1'

LAYOUT : CTH K 74



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PROJECT NO: 5034-00-72

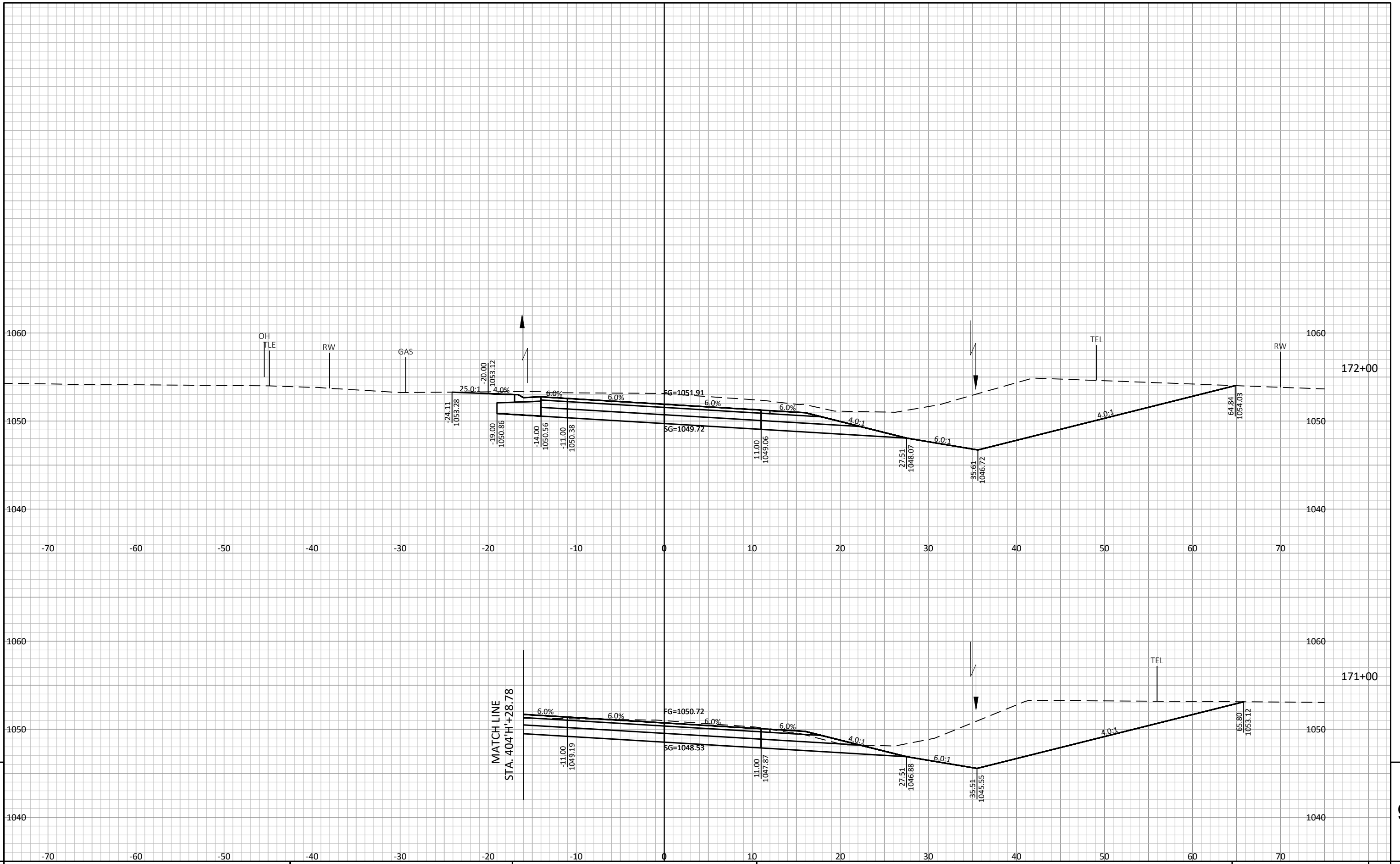
HWY: CTH F

COUNTY: IOWA

CROSS SECTIONS: MAINLINE

SHEET

E



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PROJECT NO: 5034-00-72

HWY: CTH F

COUNTY: IOWA

CROSS SECTIONS: MAINLINE

SHEET

E

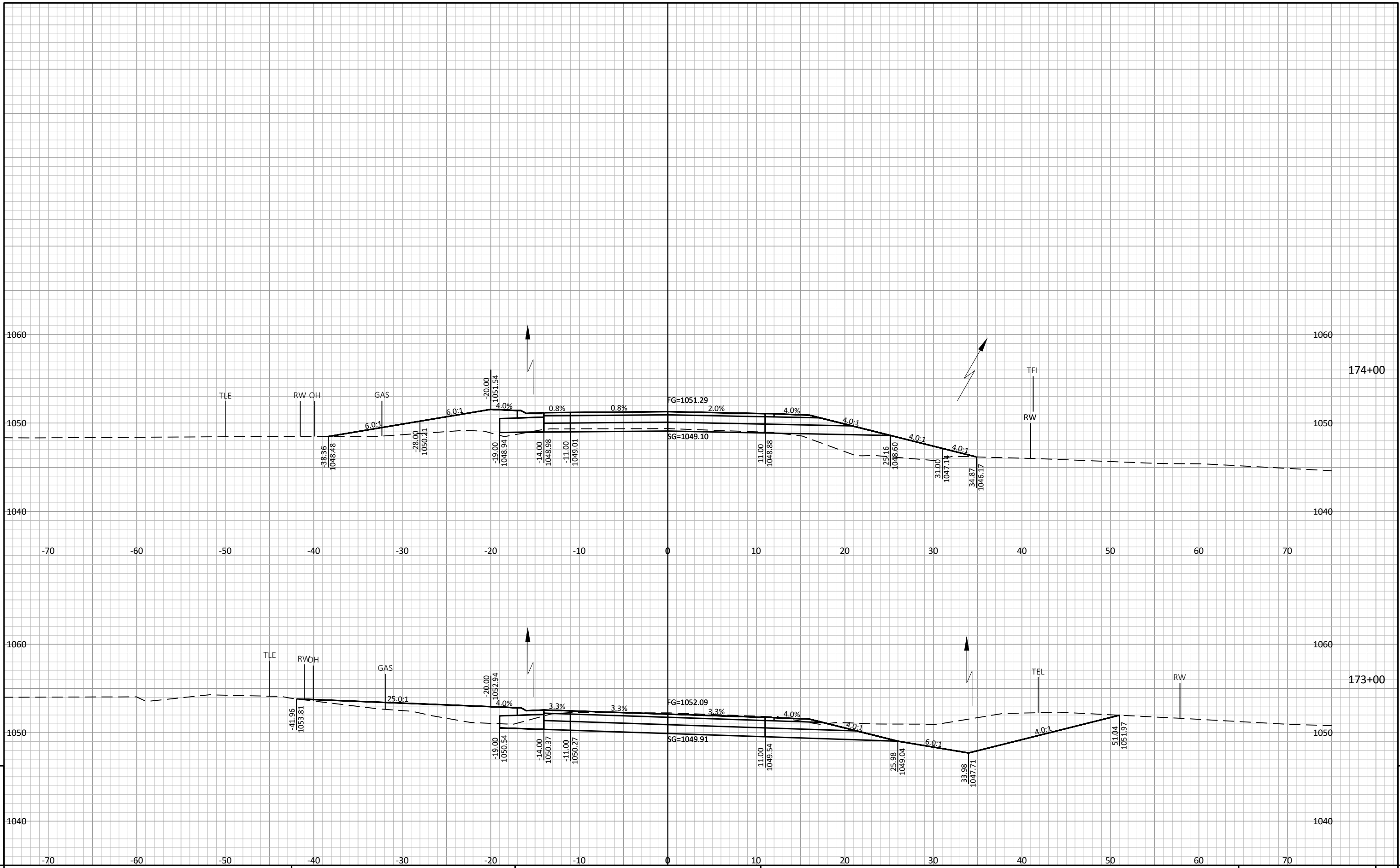
FILE NAME : S:\PROJECTS\K19250 CTH F IOWA COUNTY\DESIGN\CORRIDORS\CORRIDOR_CTH F.DWG

PLOT DATE : 9/8/2021 12:21:09 PM

PLOT BY : JONAH DRAKE

PLOT SCALE : 1" = 1'

LAYOUT : CTH K 77



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PROJECT NO: 5034-00-72

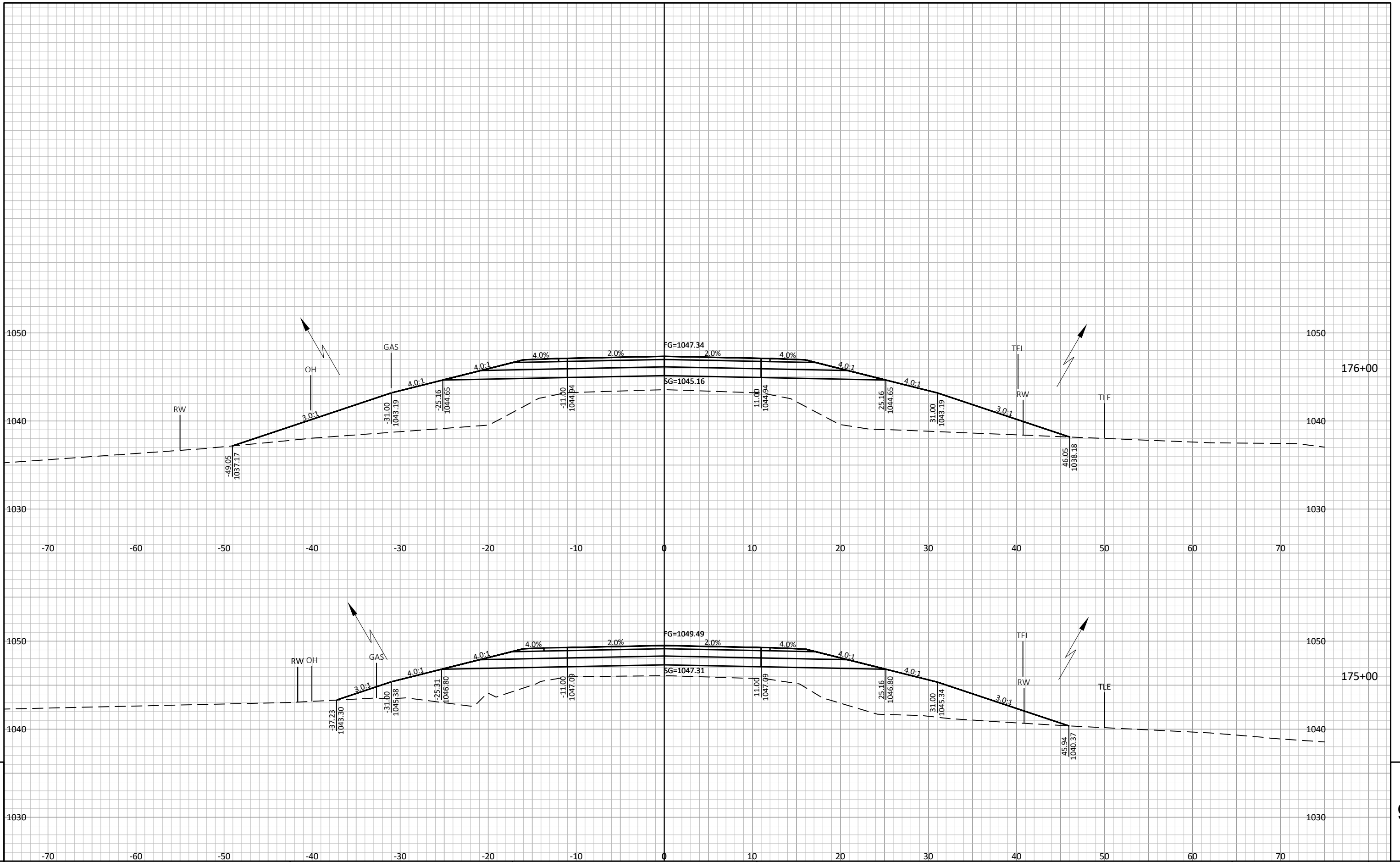
HWY: CTH F

COUNTY: IOWA

CROSS SECTIONS: MAINLINE

SHEET

E



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PROJECT NO: 5034-00-72

HWY: CTH F

COUNTY: IOWA

CROSS SECTIONS: MAINLINE

SHEET

E

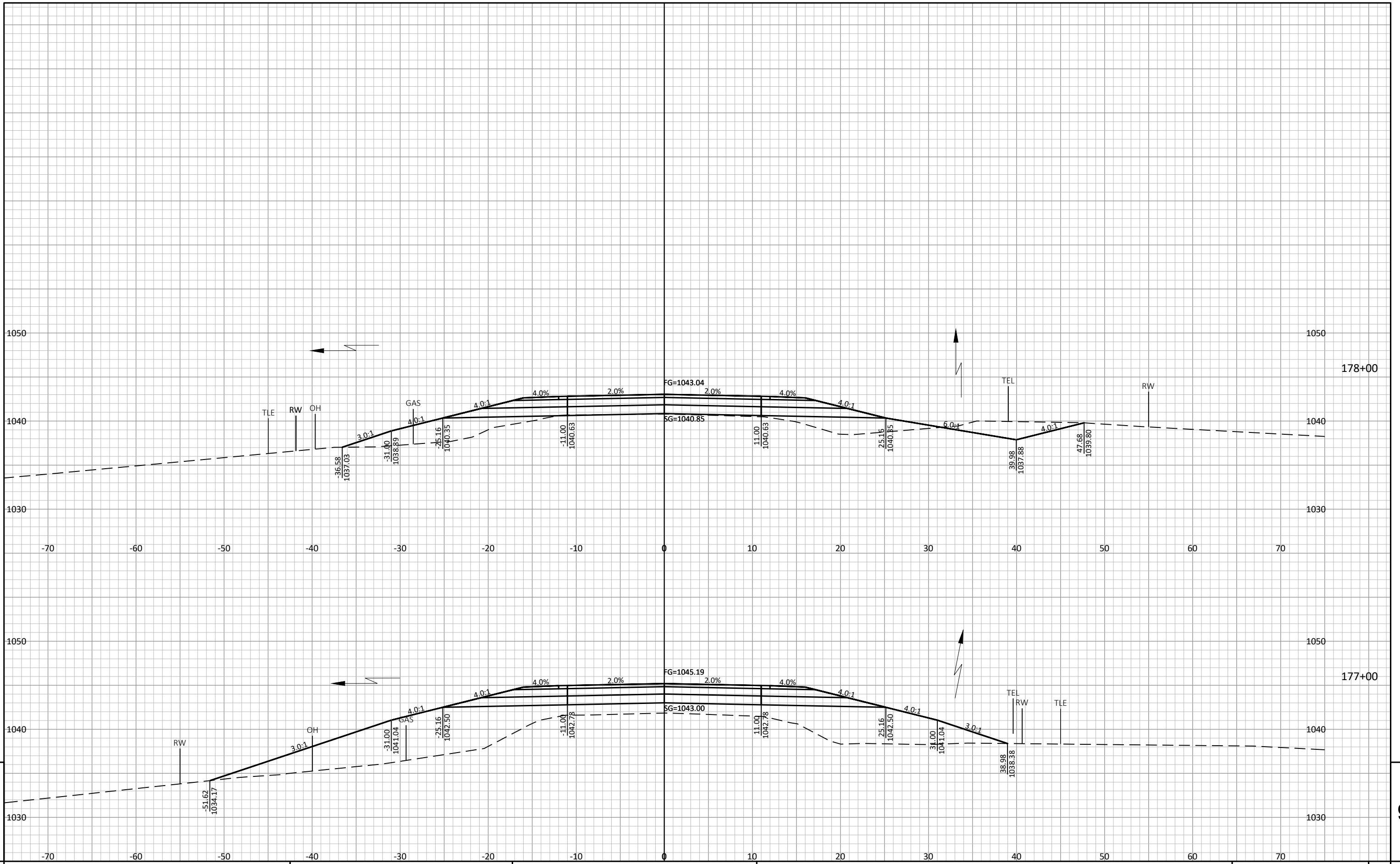
FILE NAME: S:\PROJECTS\K19250 CTH F IOWA COUNTY\DESIGN\CORRIDORS\CORRIDOR_CTH F.DWG

PLOT DATE: 9/8/2021 12:21:33 PM

PLOT BY: JONAH DRAKE

PLOT SCALE: 1" = 1'

LAYOUT: CTH K 79



PROJECT NO: 5034-00-72

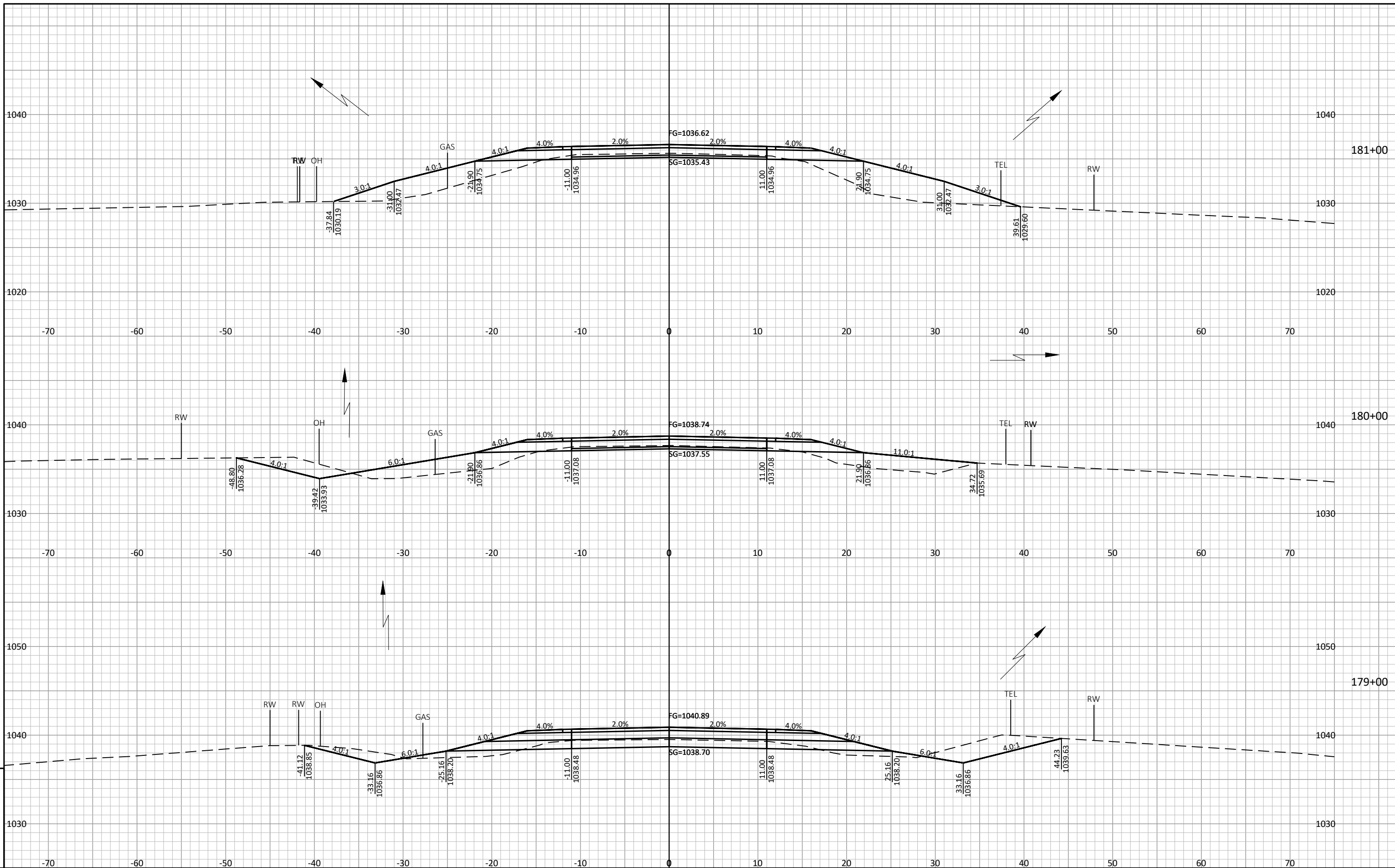
HWY: CTH F

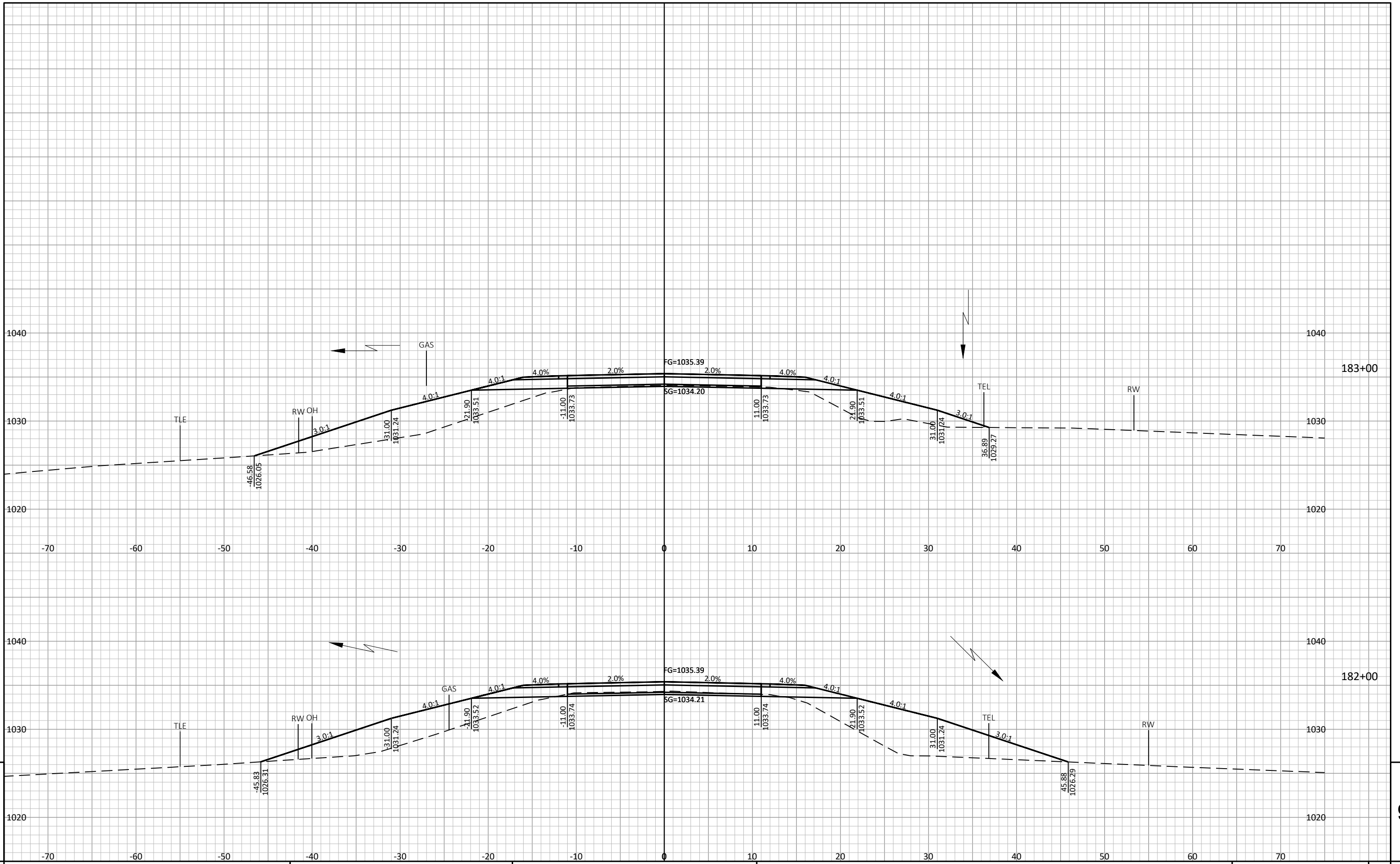
COUNTY: IOWA

CROSS SECTIONS: MAINLINE

SHEET

E





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PROJECT NO: 5034-00-72

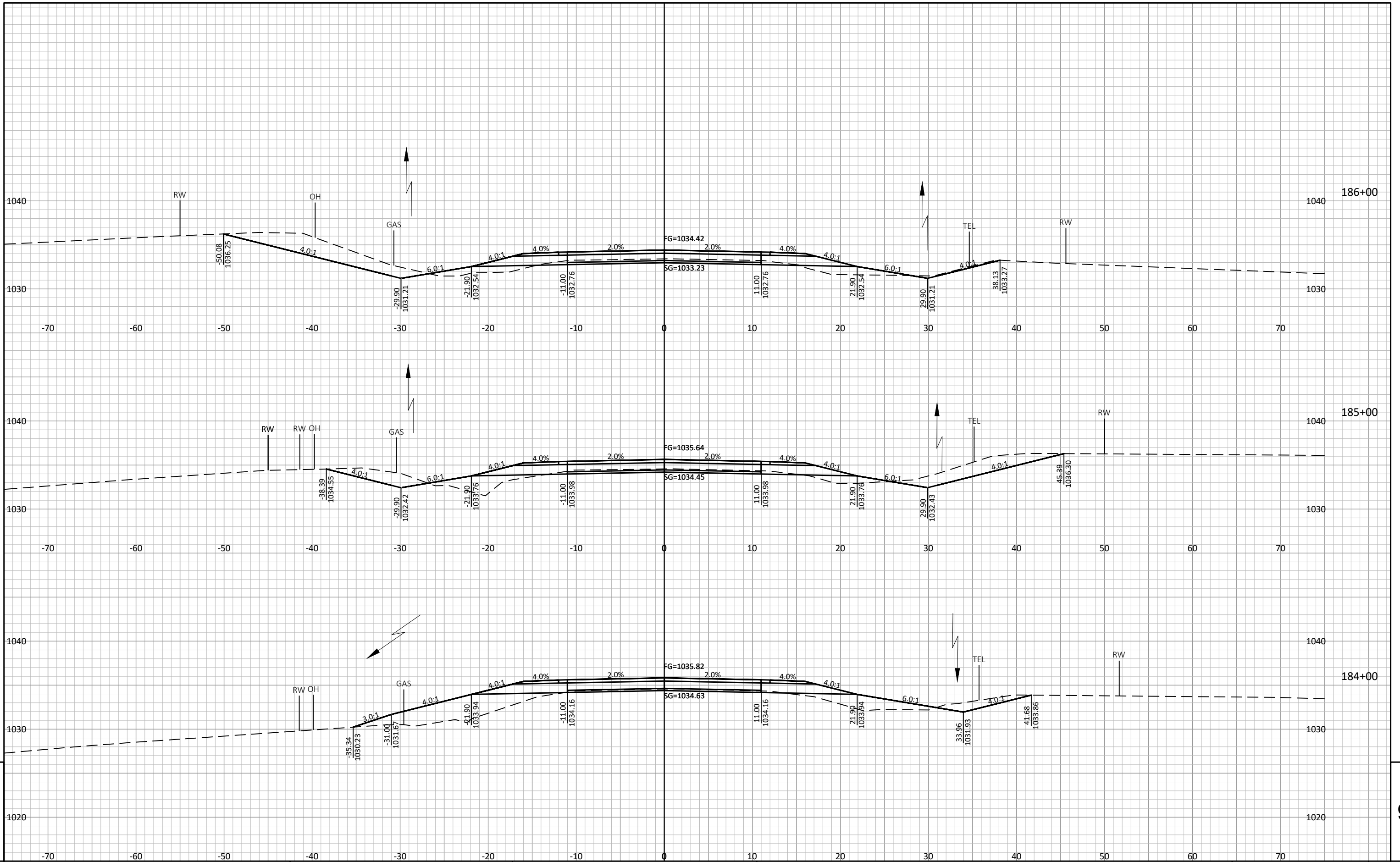
HWY: CTH F

COUNTY: IOWA

CROSS SECTIONS: MAINLINE

SHEET

E



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PROJECT NO: 5034-00-72

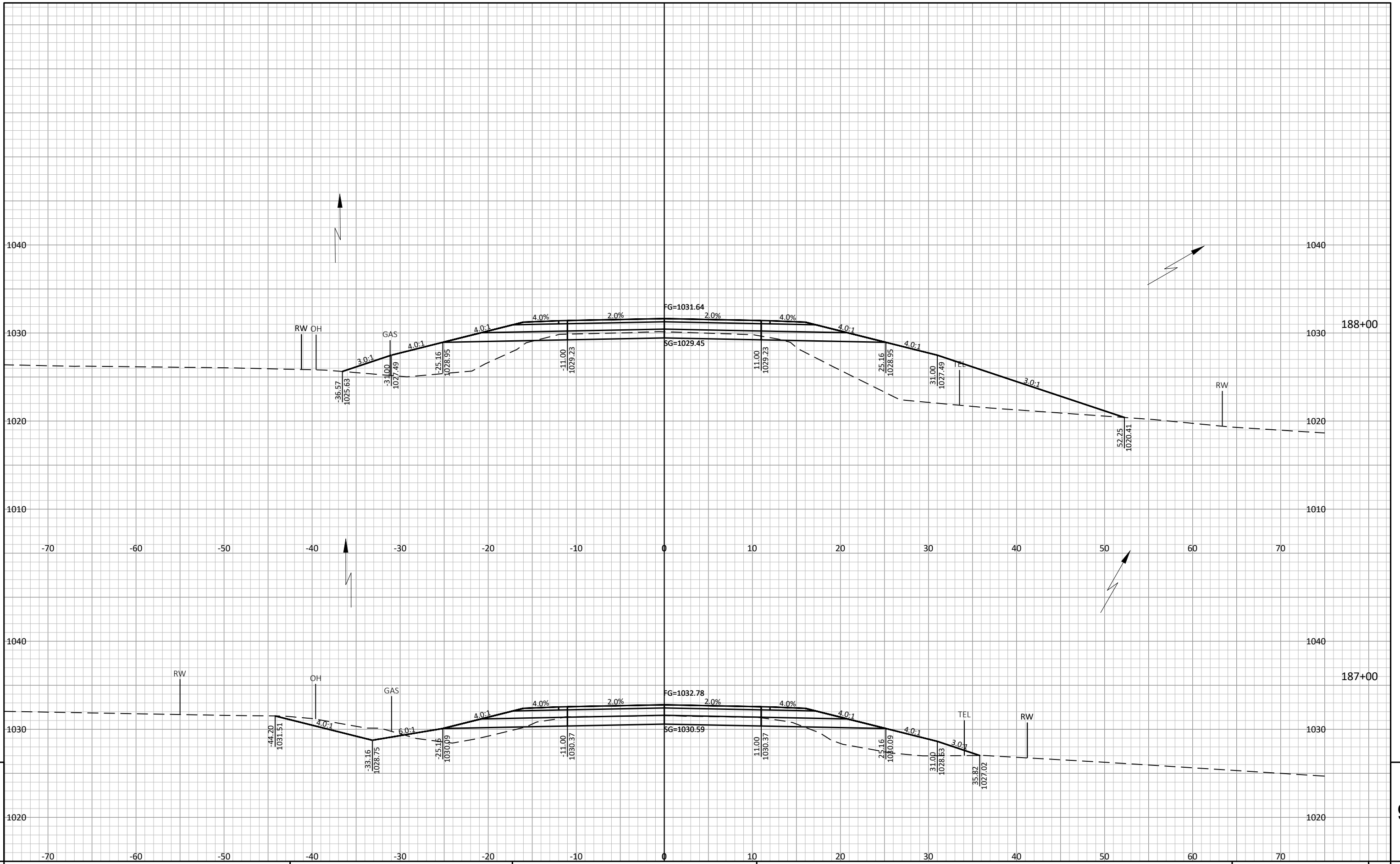
HWY: CTH F

COUNTY: IOWA

CROSS SECTIONS: MAINLINE

SHEET

E



PROJECT NO: 5034-00-72

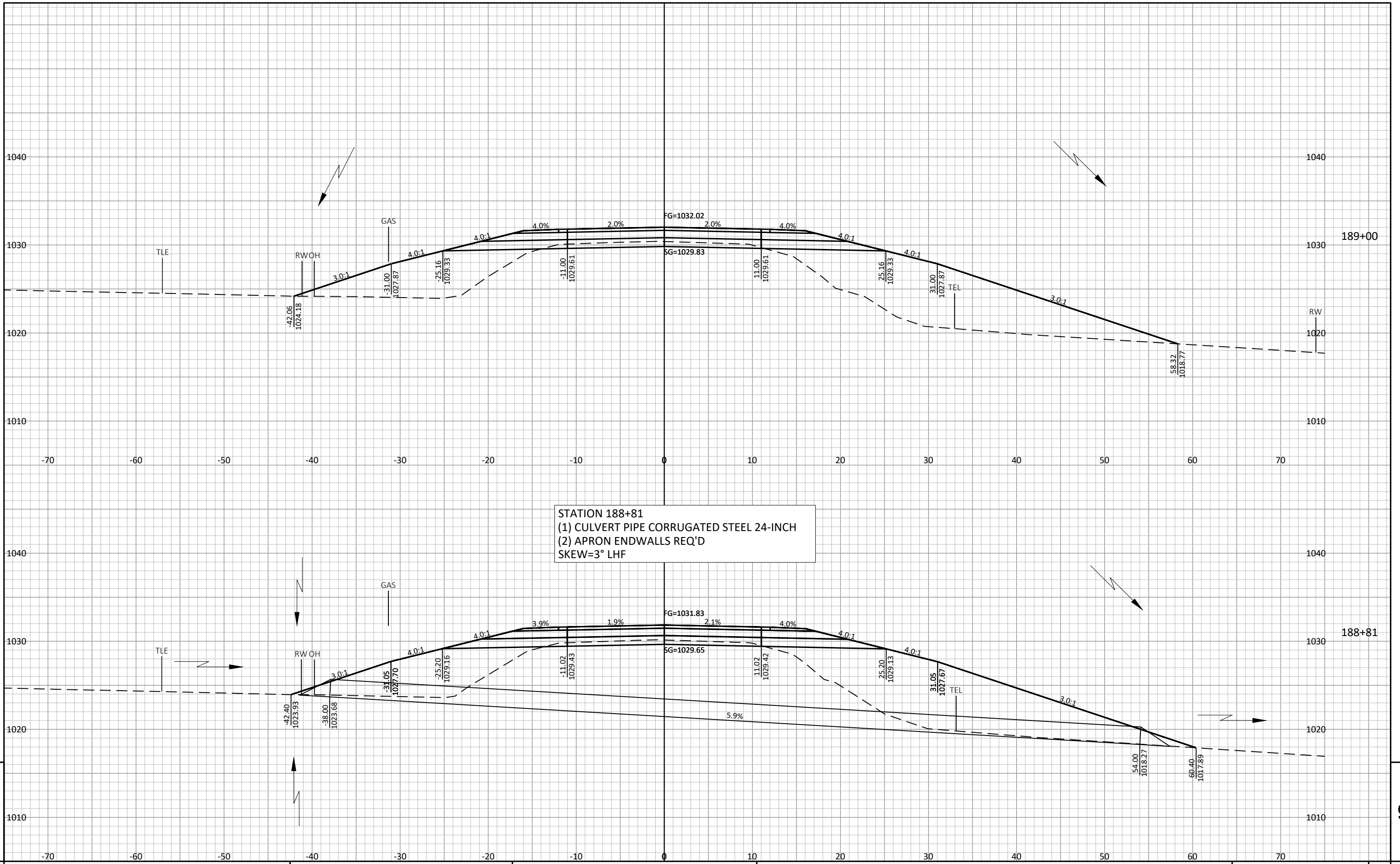
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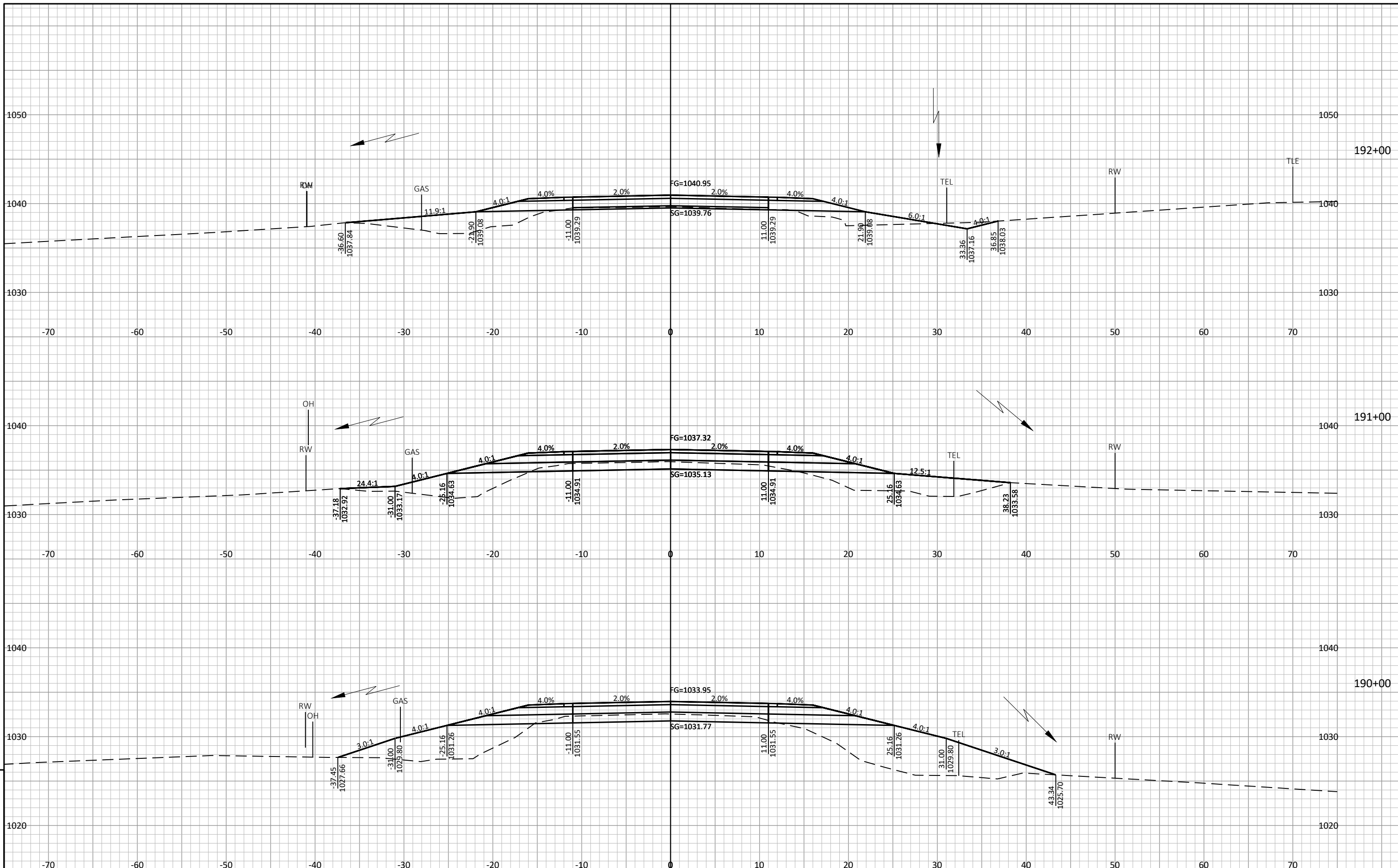
COUNTY: IOWA

CROSS SECTIONS: MAINLINE

SHEET

E





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PROJECT NO: 5034-00-72

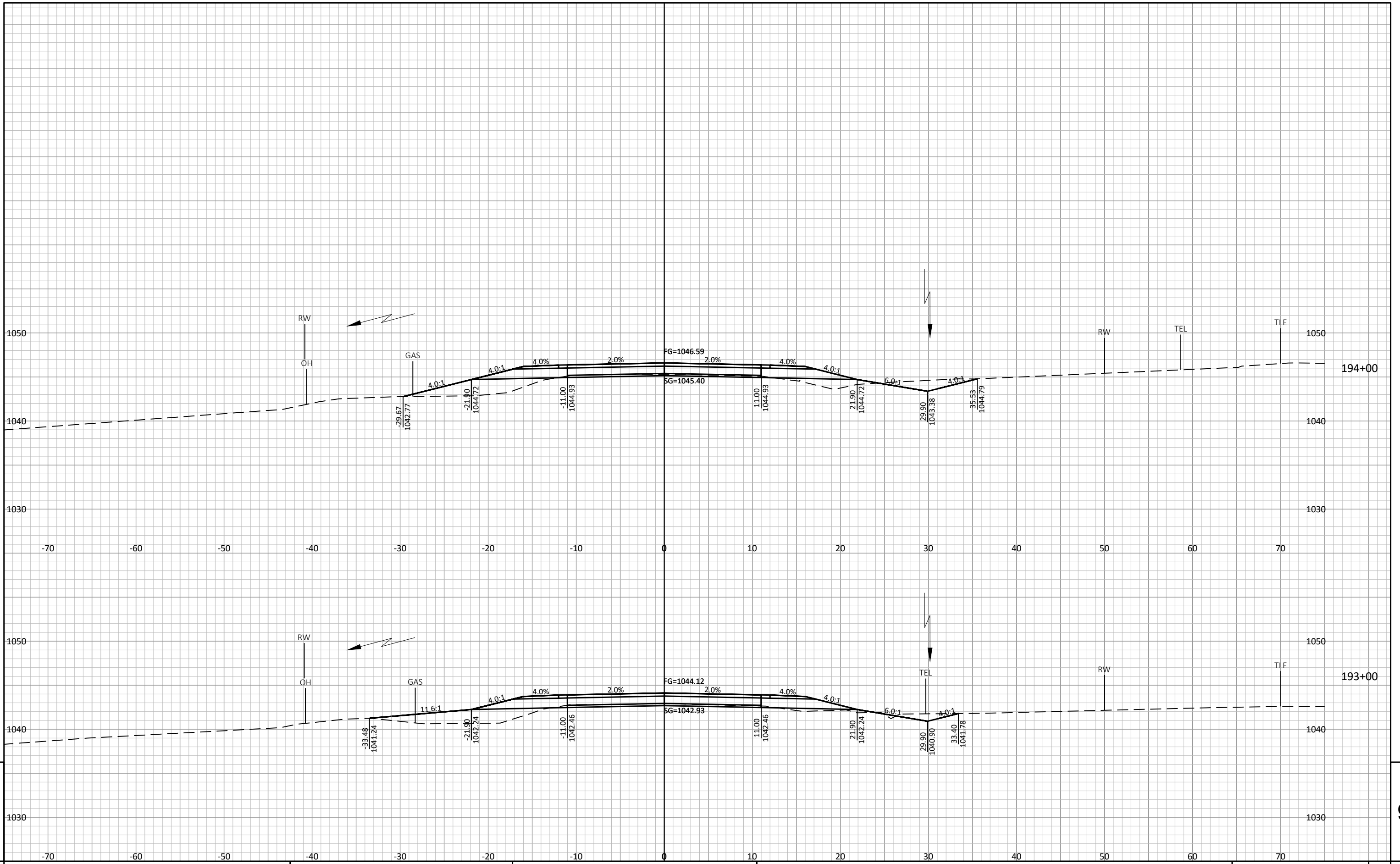
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COUNTY: IOWA

CROSS SECTIONS: MAINLINE

SHEET

E



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PROJECT NO: 5034-00-72

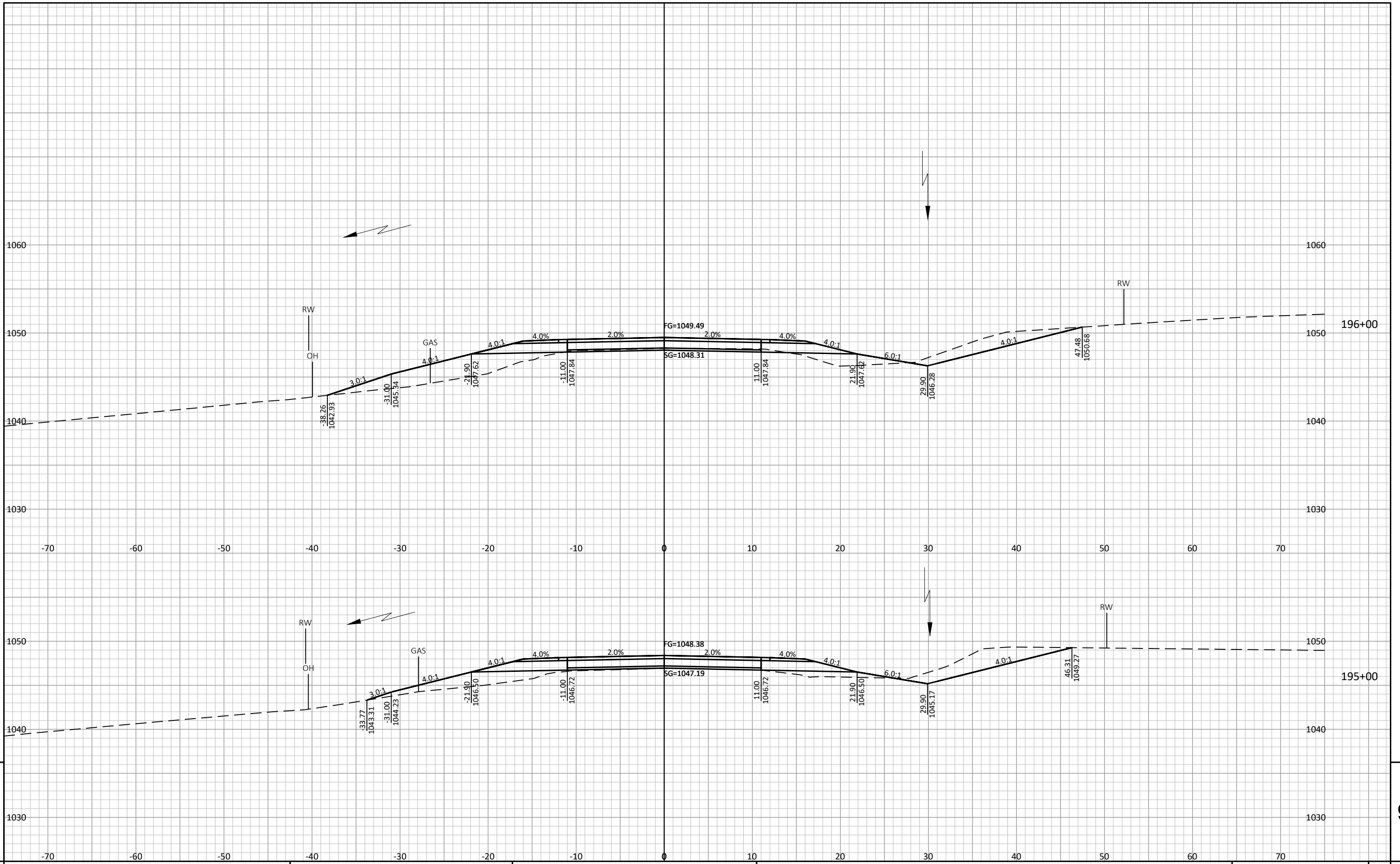
HWY: CTH F

COUNTY: IOWA

CROSS SECTIONS: MAINLINE

SHEET

E



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9

PROJECT NO: 5034-00-72

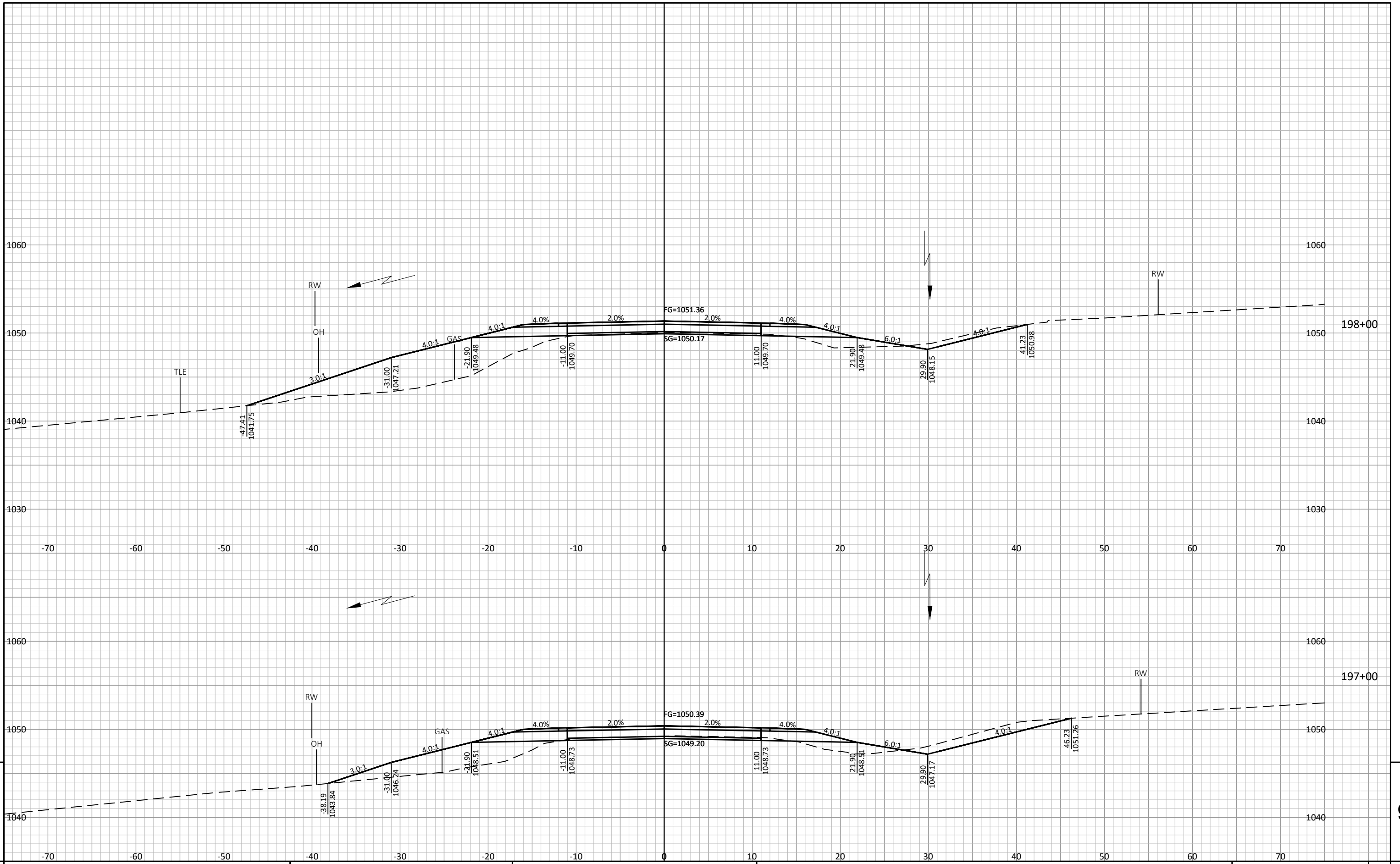
HWY: CTH F

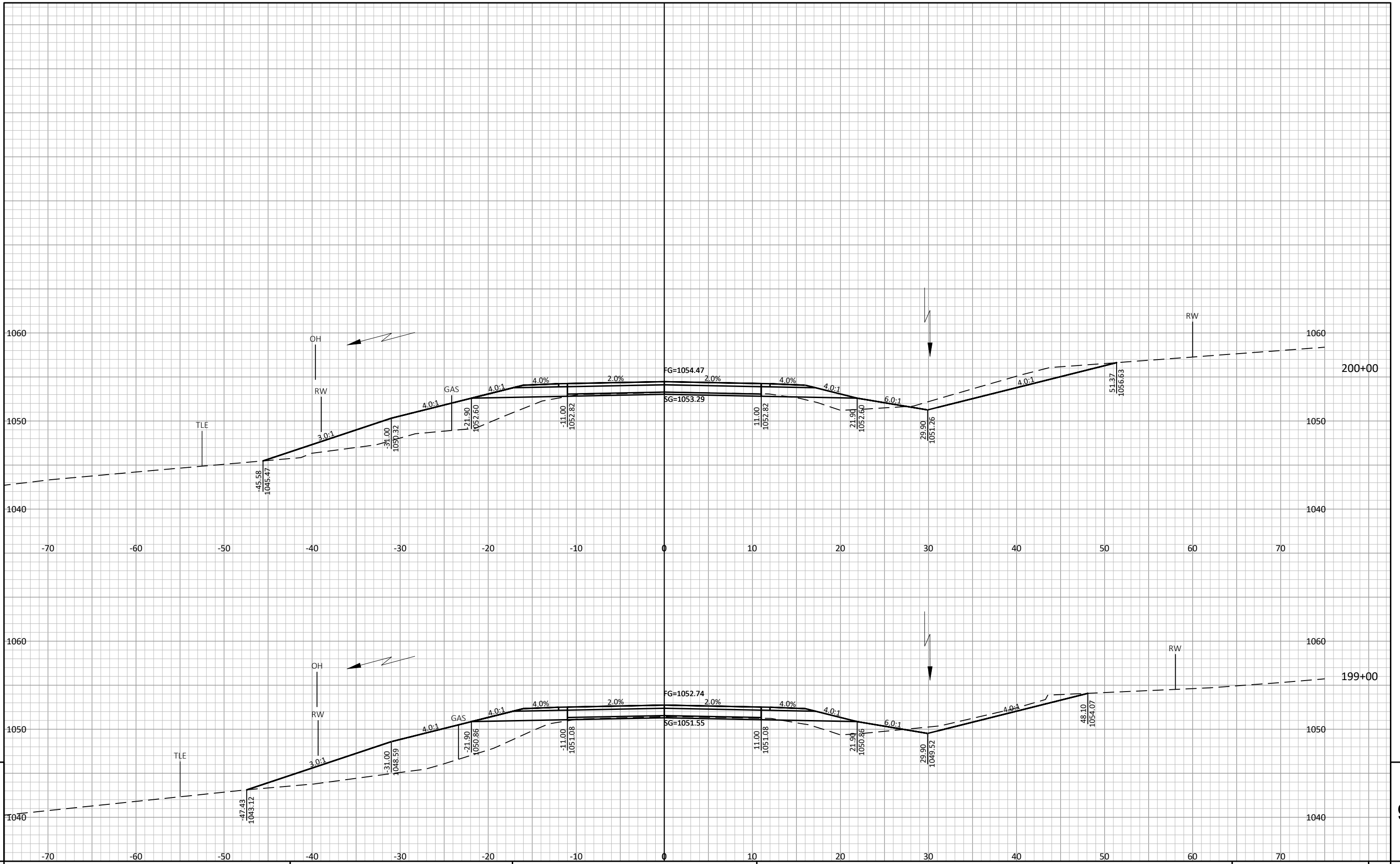
COUNTY: IOWA

CROSS SECTIONS: MAINLINE

SHEET

E





PROJECT NO: 5034-00-72

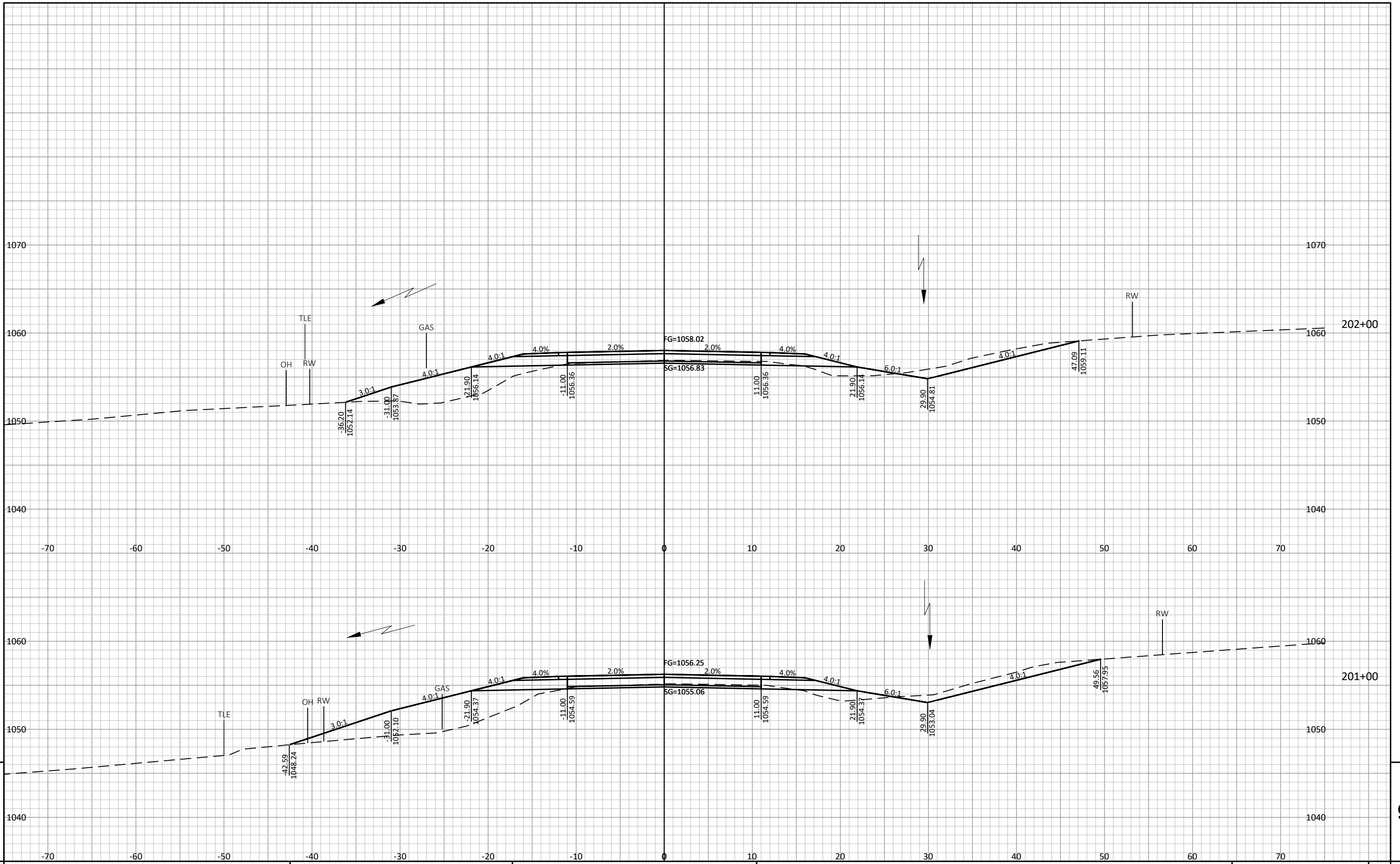
HWY: CTH F

COUNTY: IOWA

CROSS SECTIONS: MAINLINE

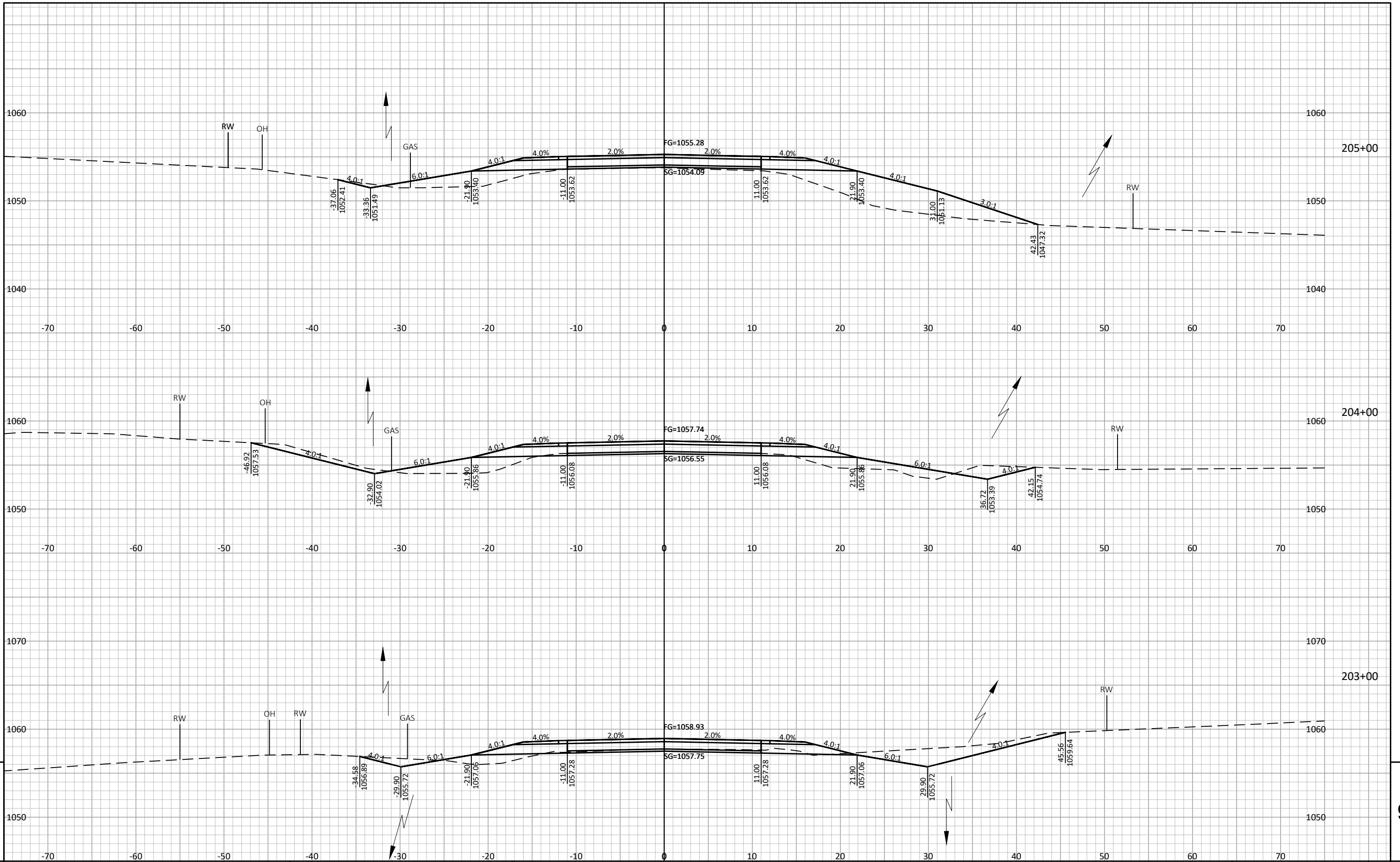
SHEET

E



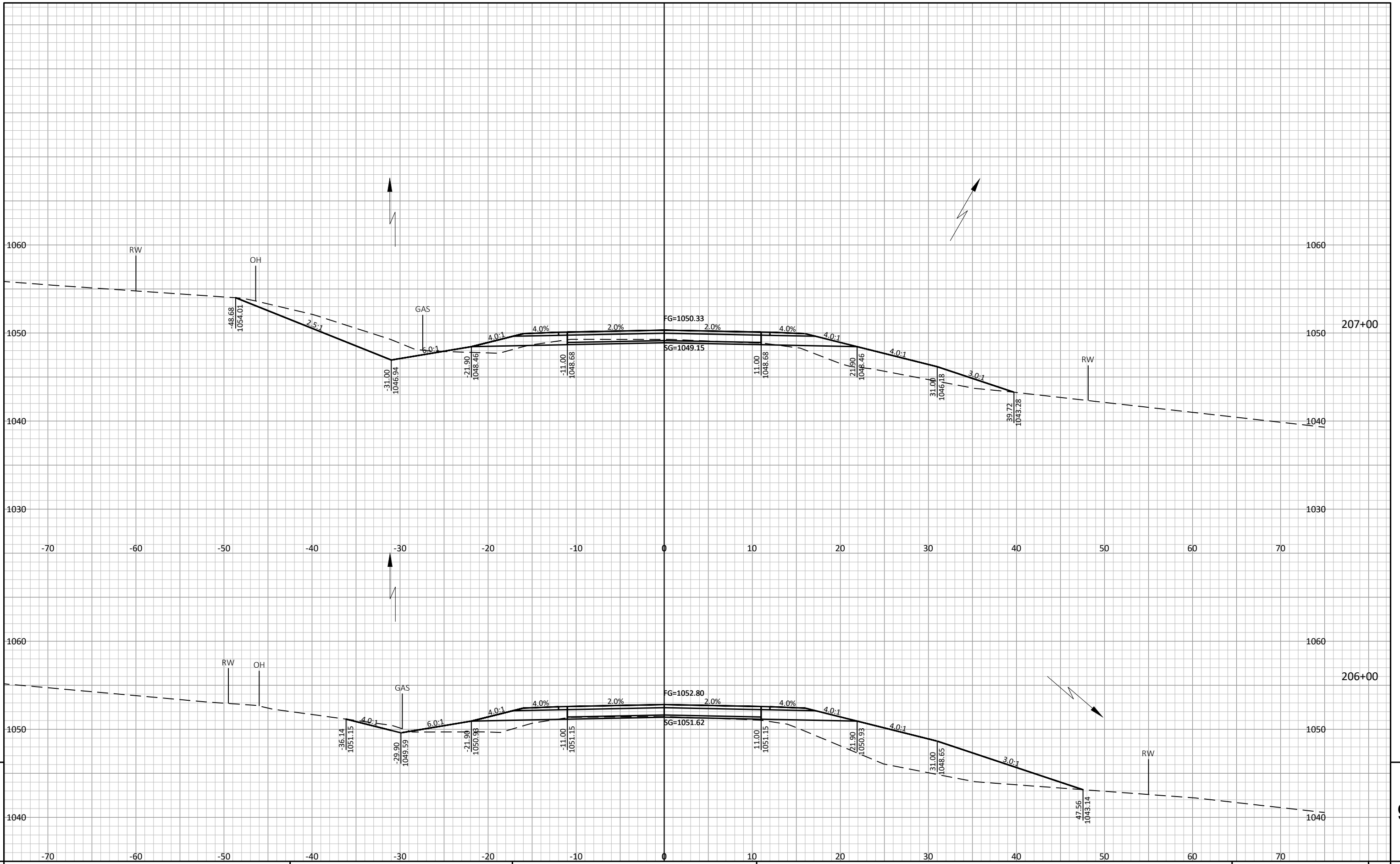
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PROJECT NO: 5034-00-72

HWY: CTH F

COUNTY: IOWA

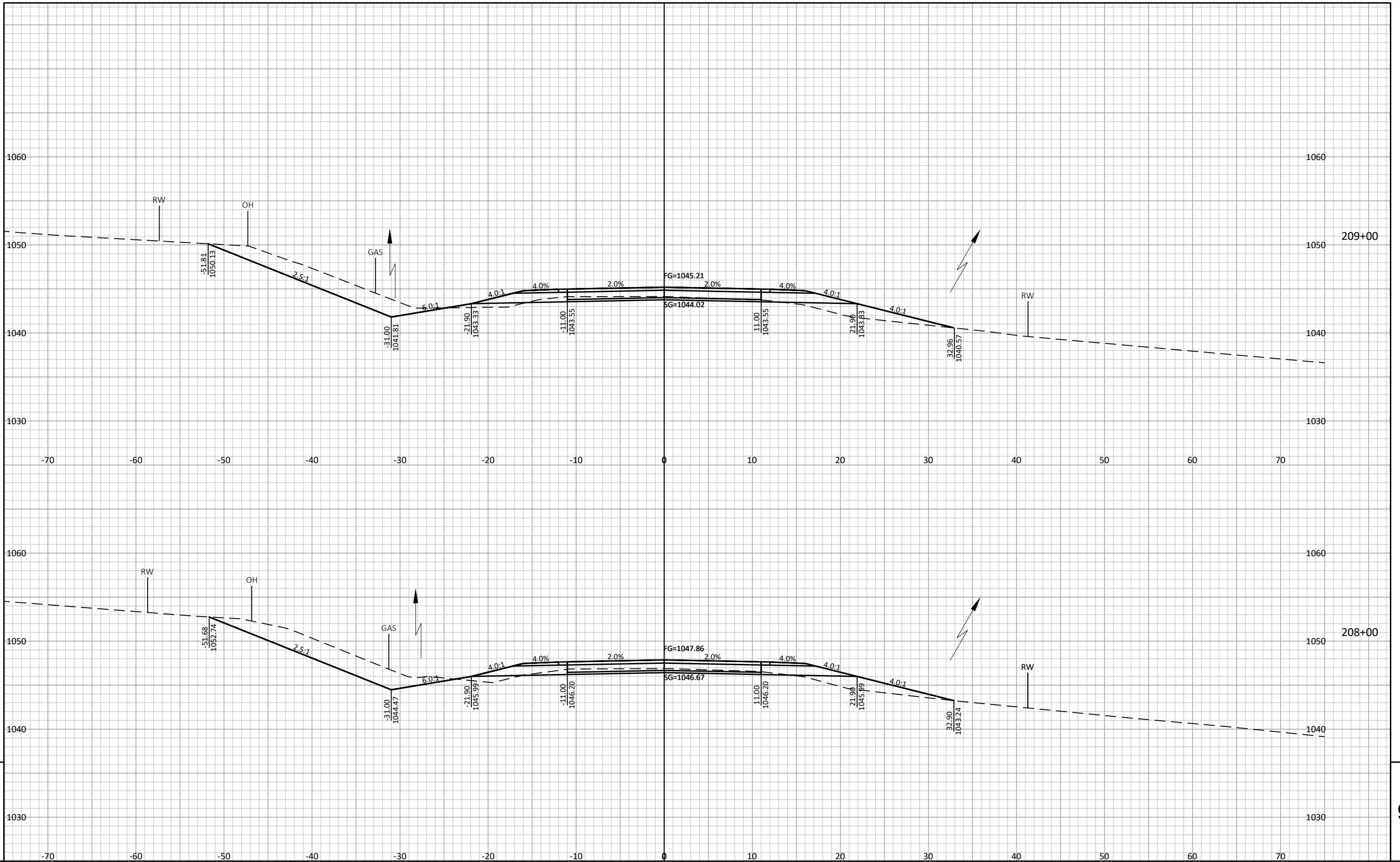
CROSS SECTIONS: MAINLINE

SHEET

E

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PROJECT NO: 5034-00-72

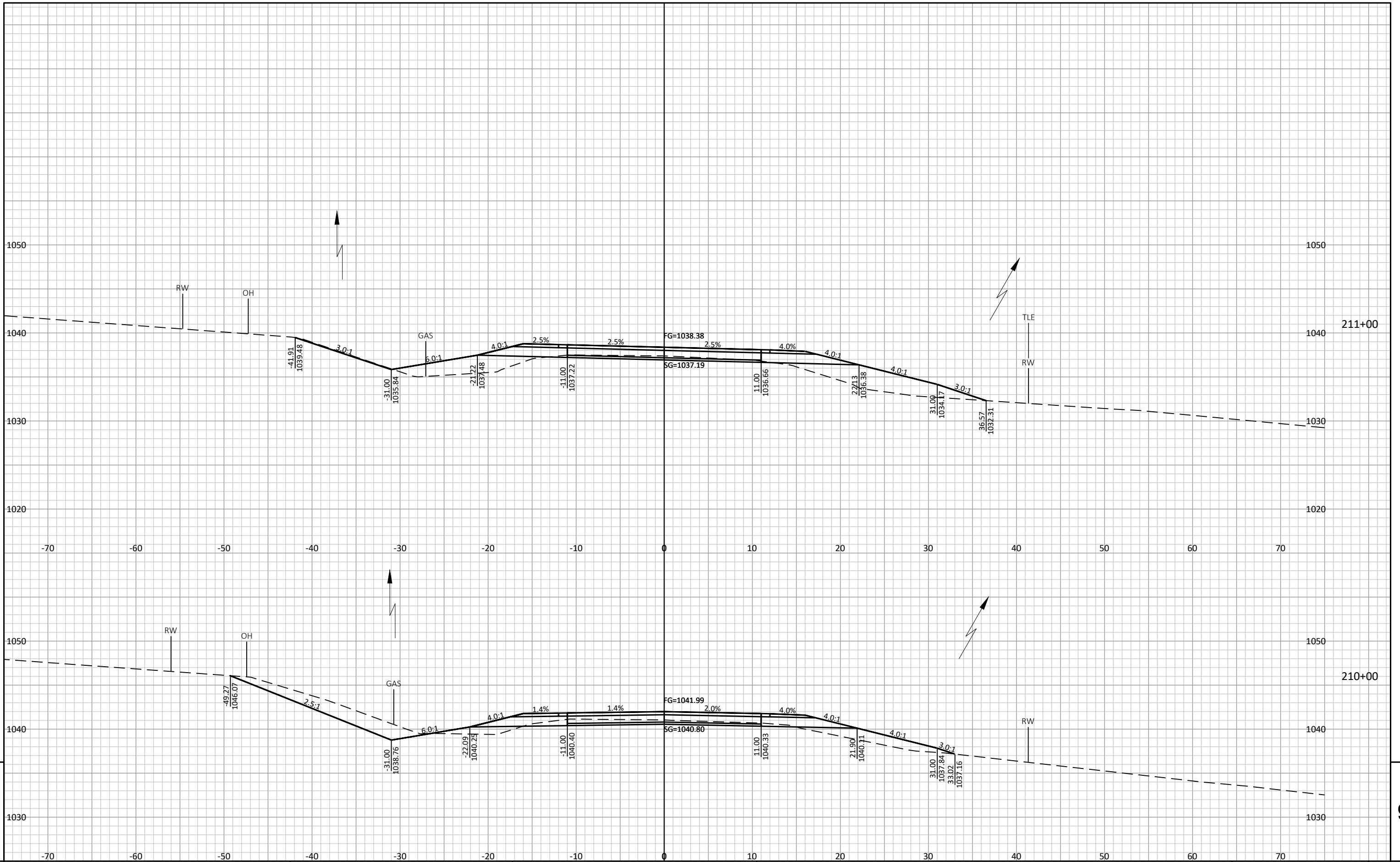
HWY: CTH F

COUNTY: IOWA

CROSS SECTIONS: MAINLINE

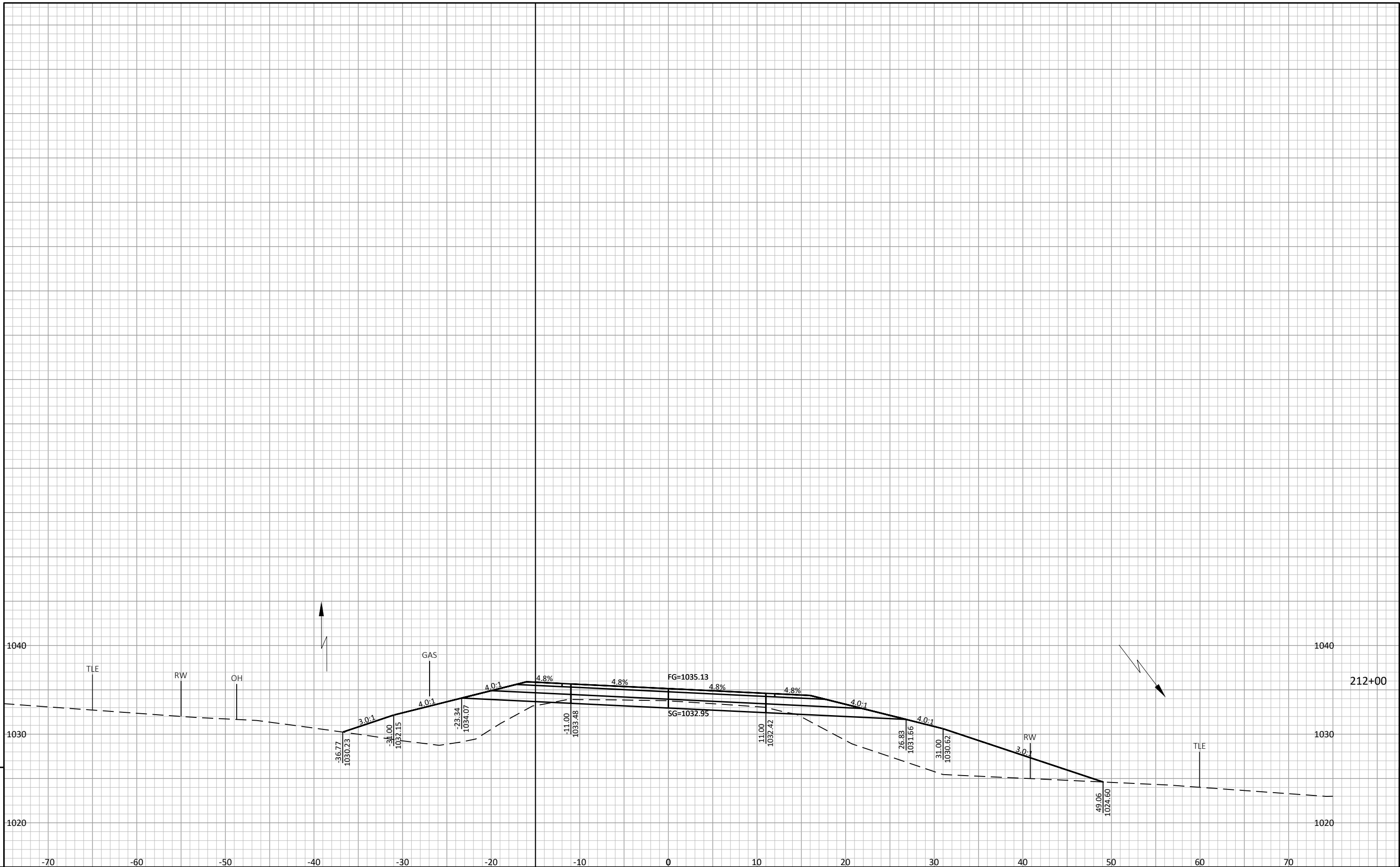
SHEET

E



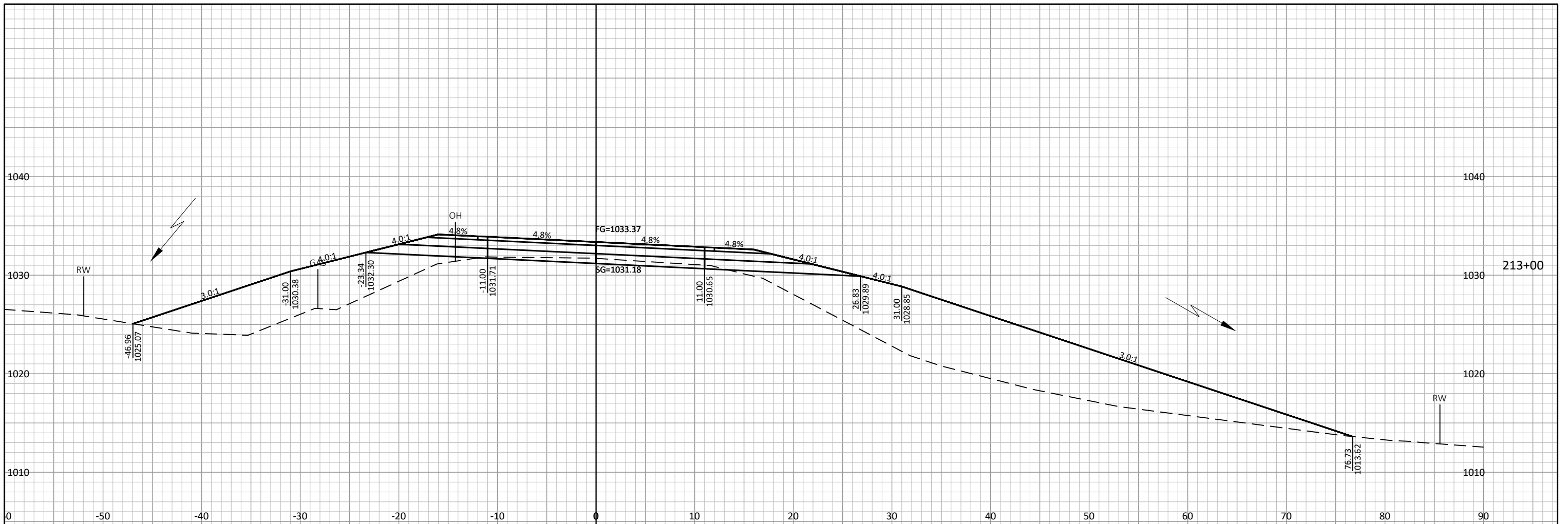
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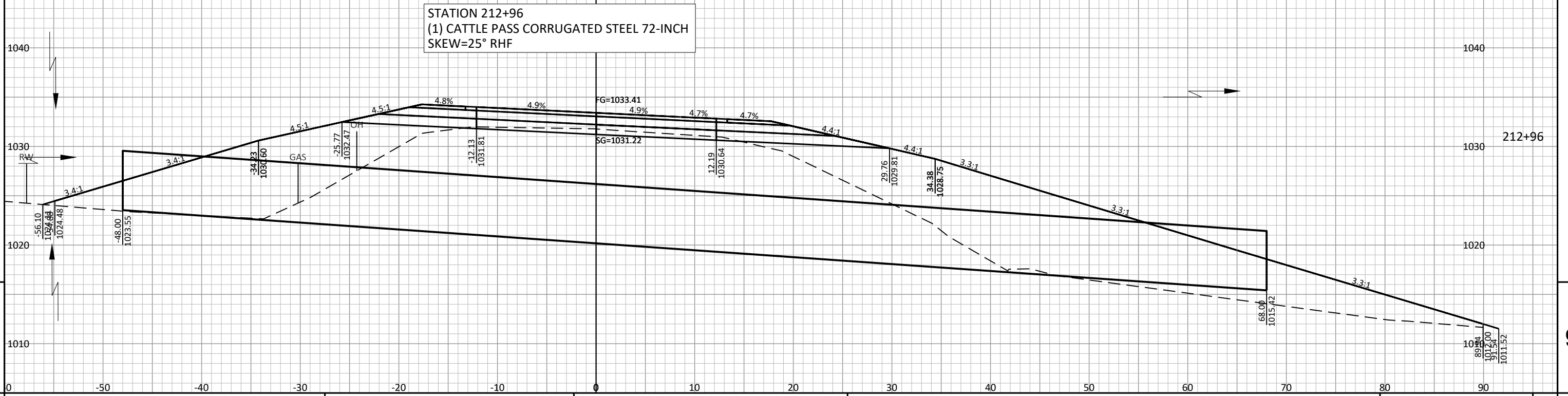


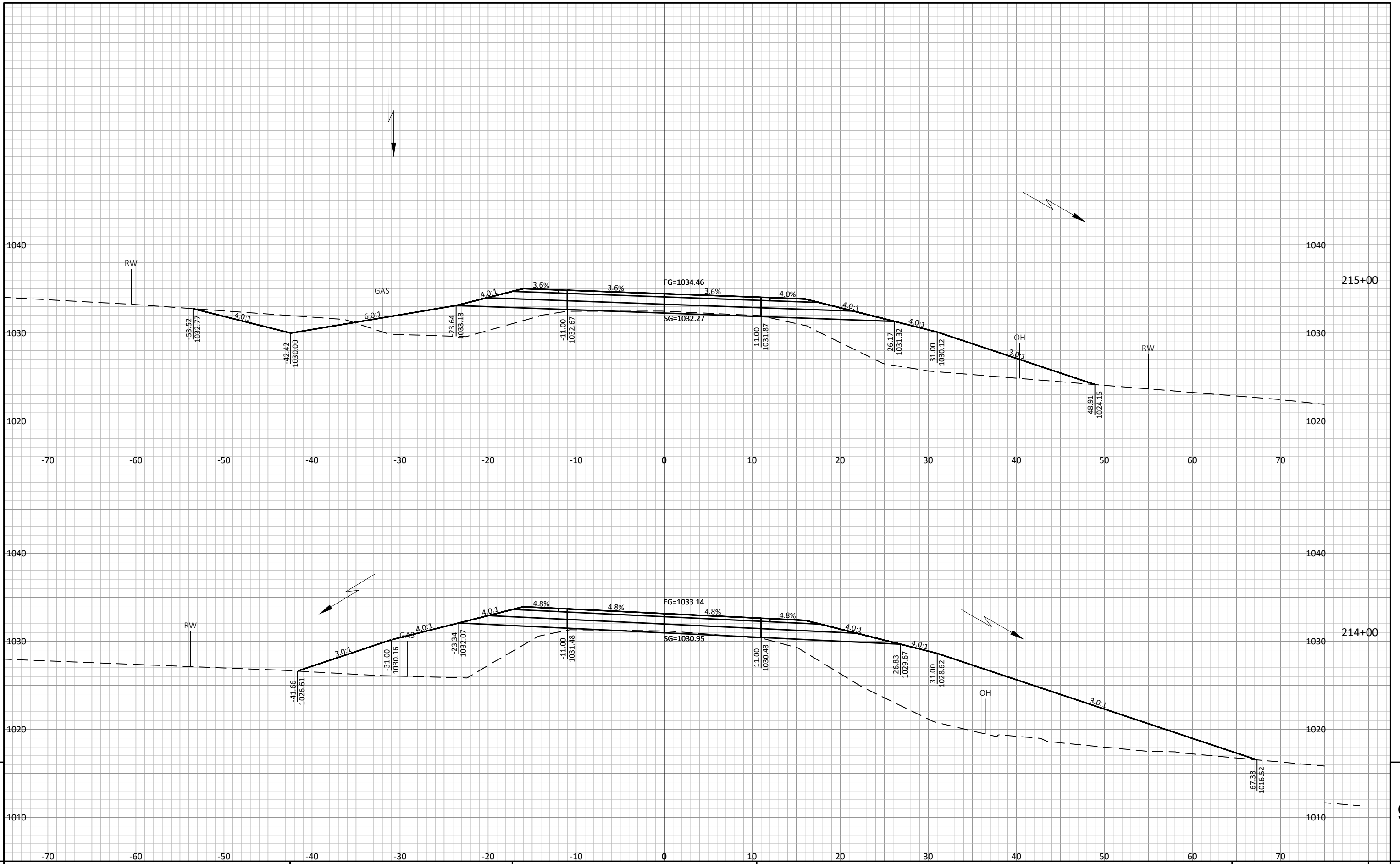
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STATION 212+96
 (1) CATTLE PASS CORRUGATED STEEL 72-INCH
 SKEW=25° RHF





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PROJECT NO: 5034-00-72

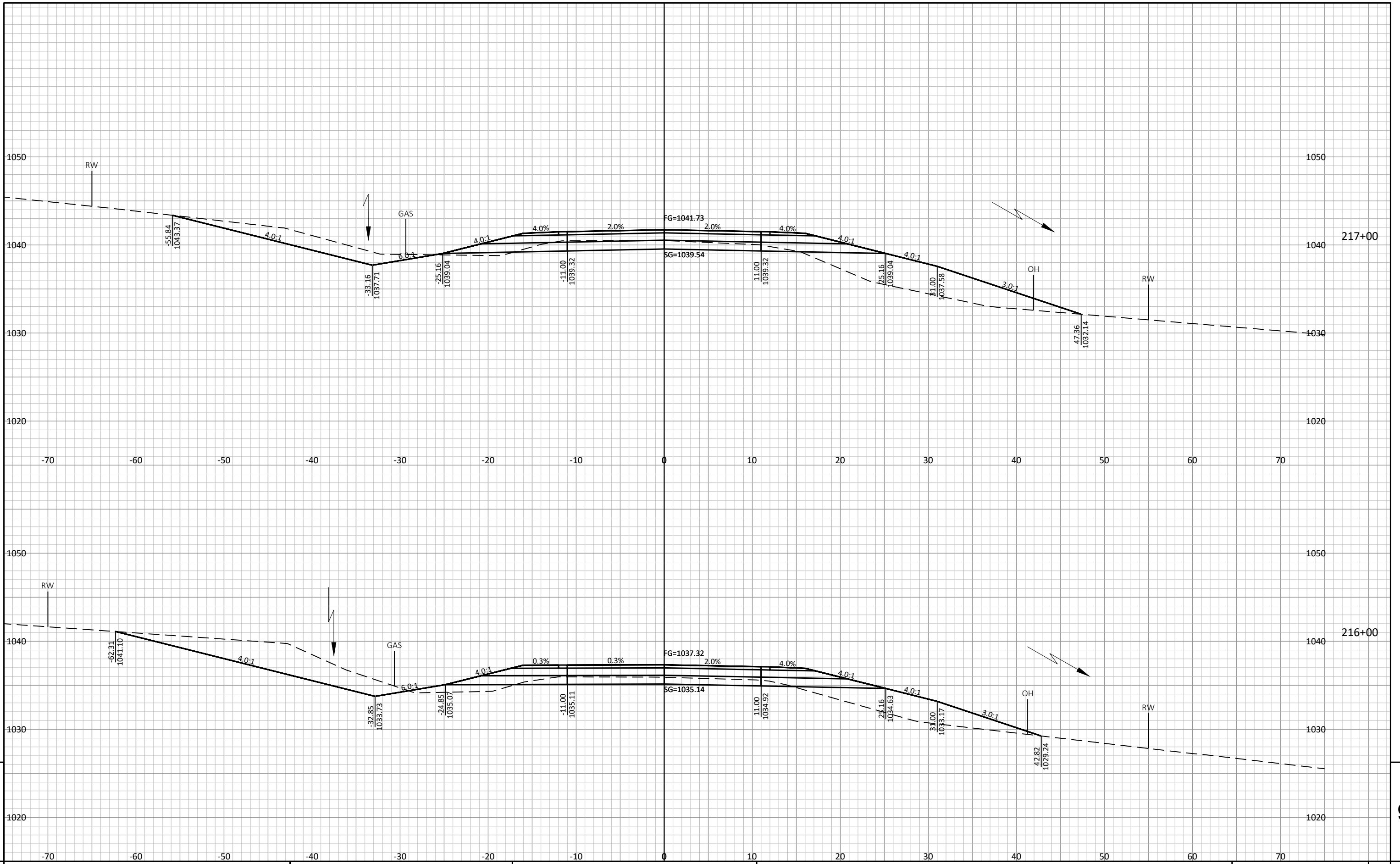
HWY: CTH F

COUNTY: IOWA

CROSS SECTIONS: MAINLINE

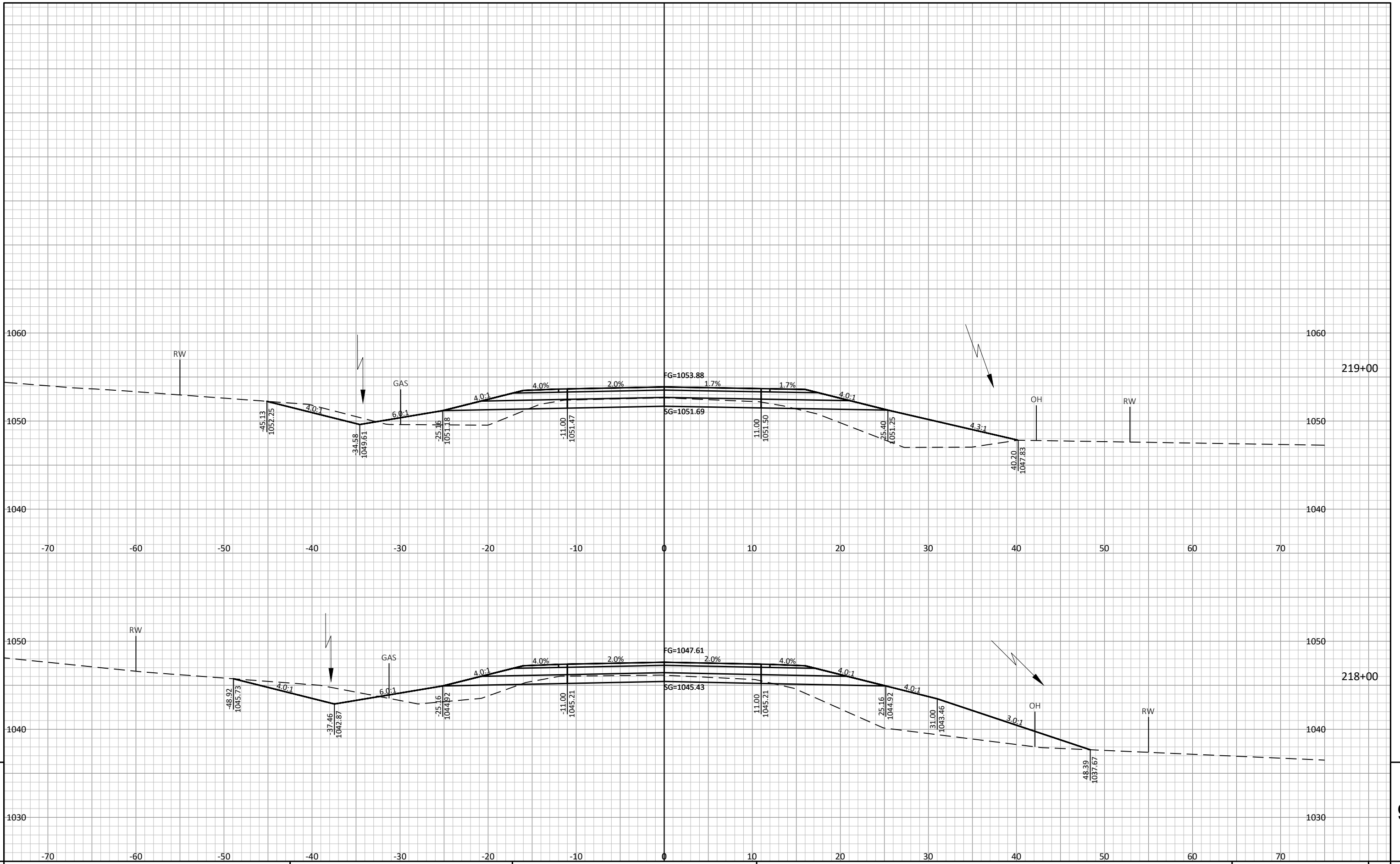
SHEET

E



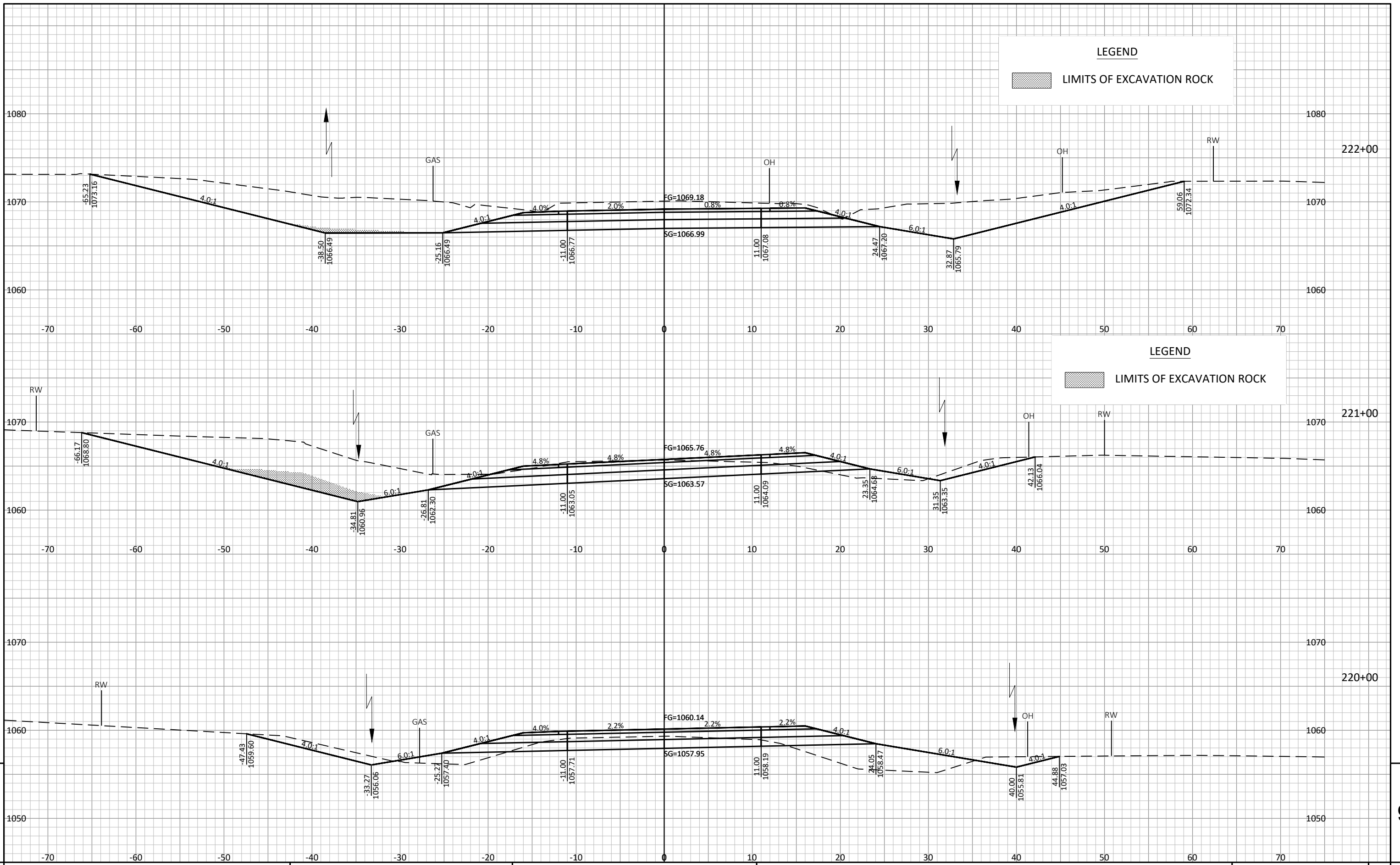
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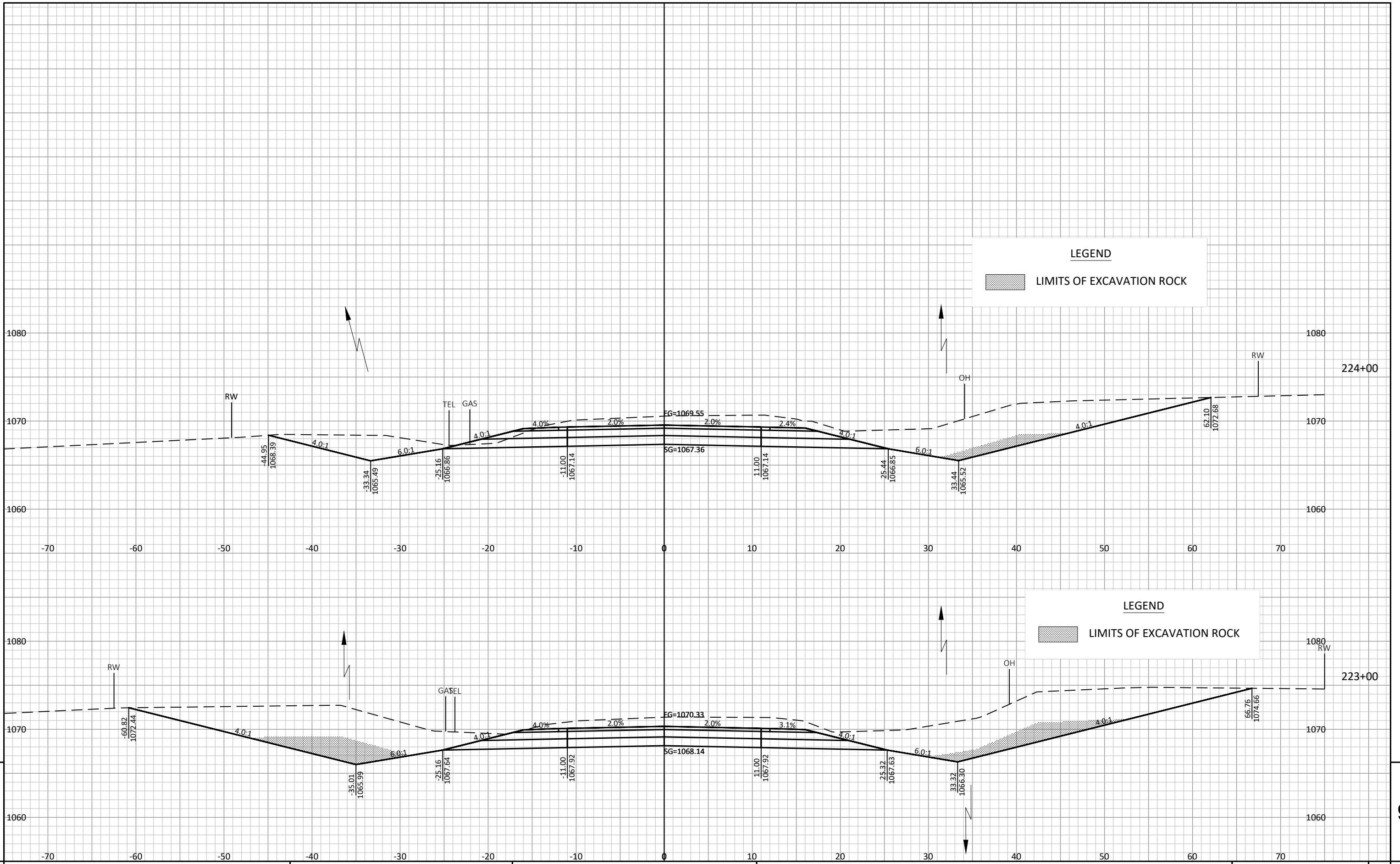
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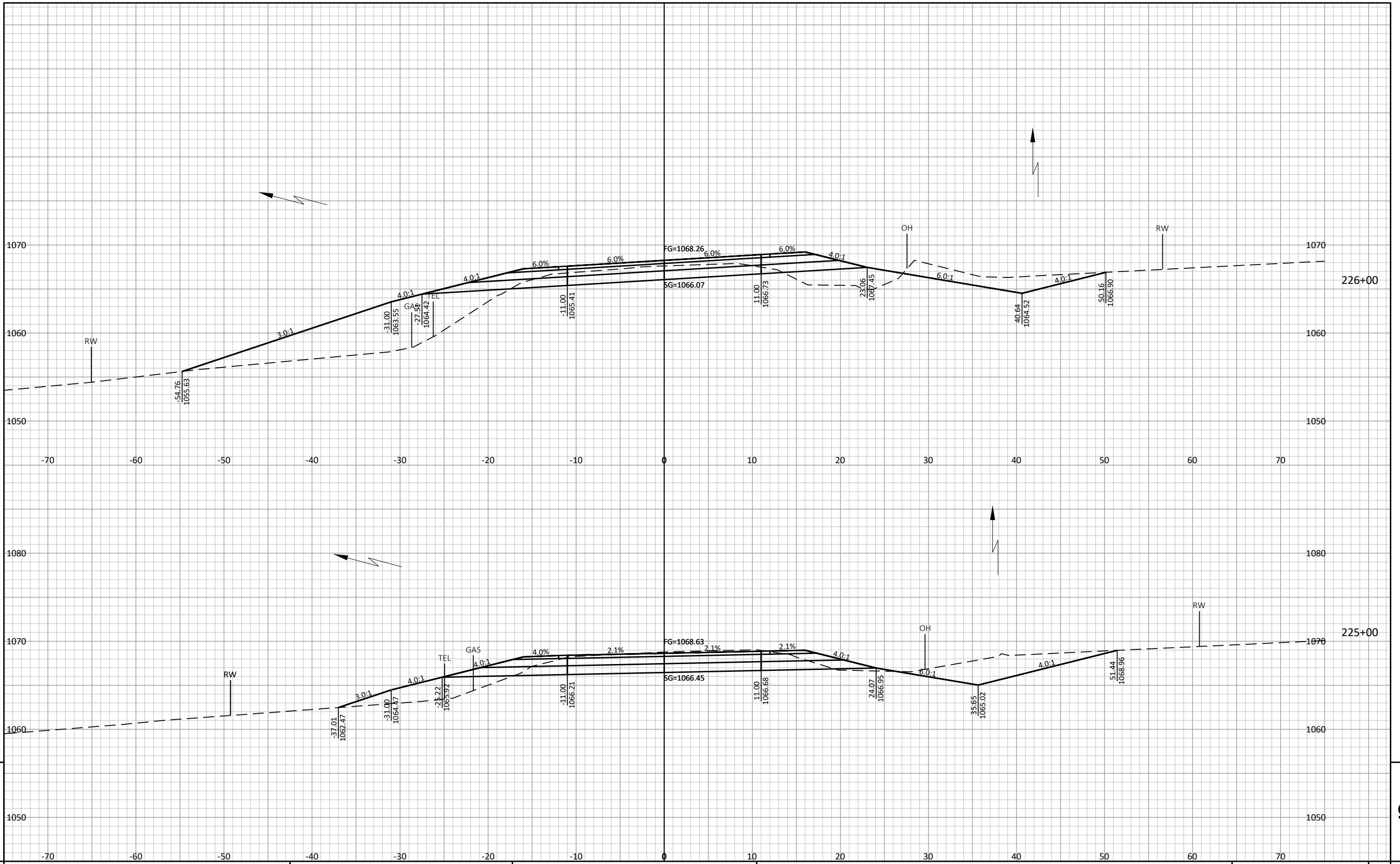
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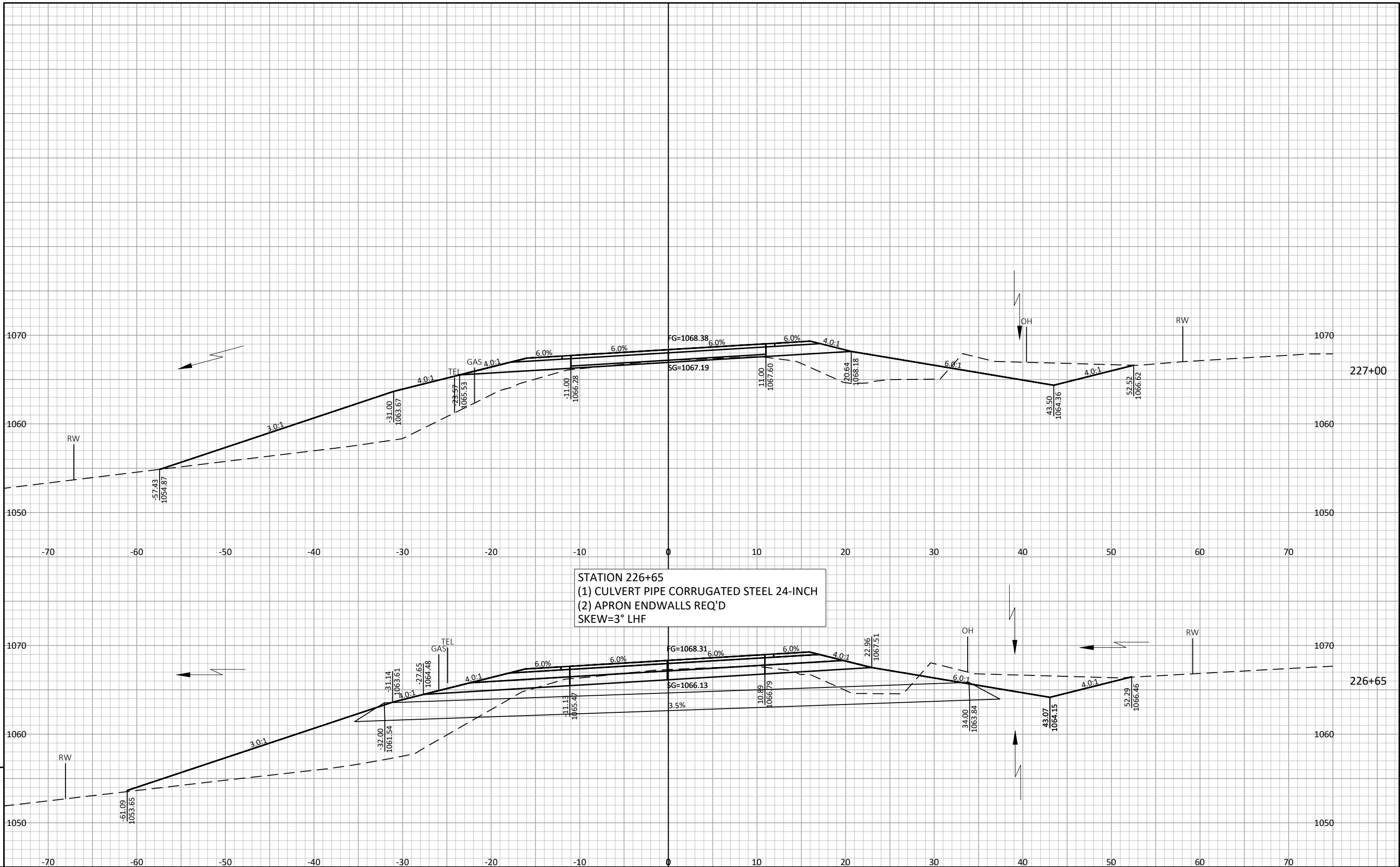
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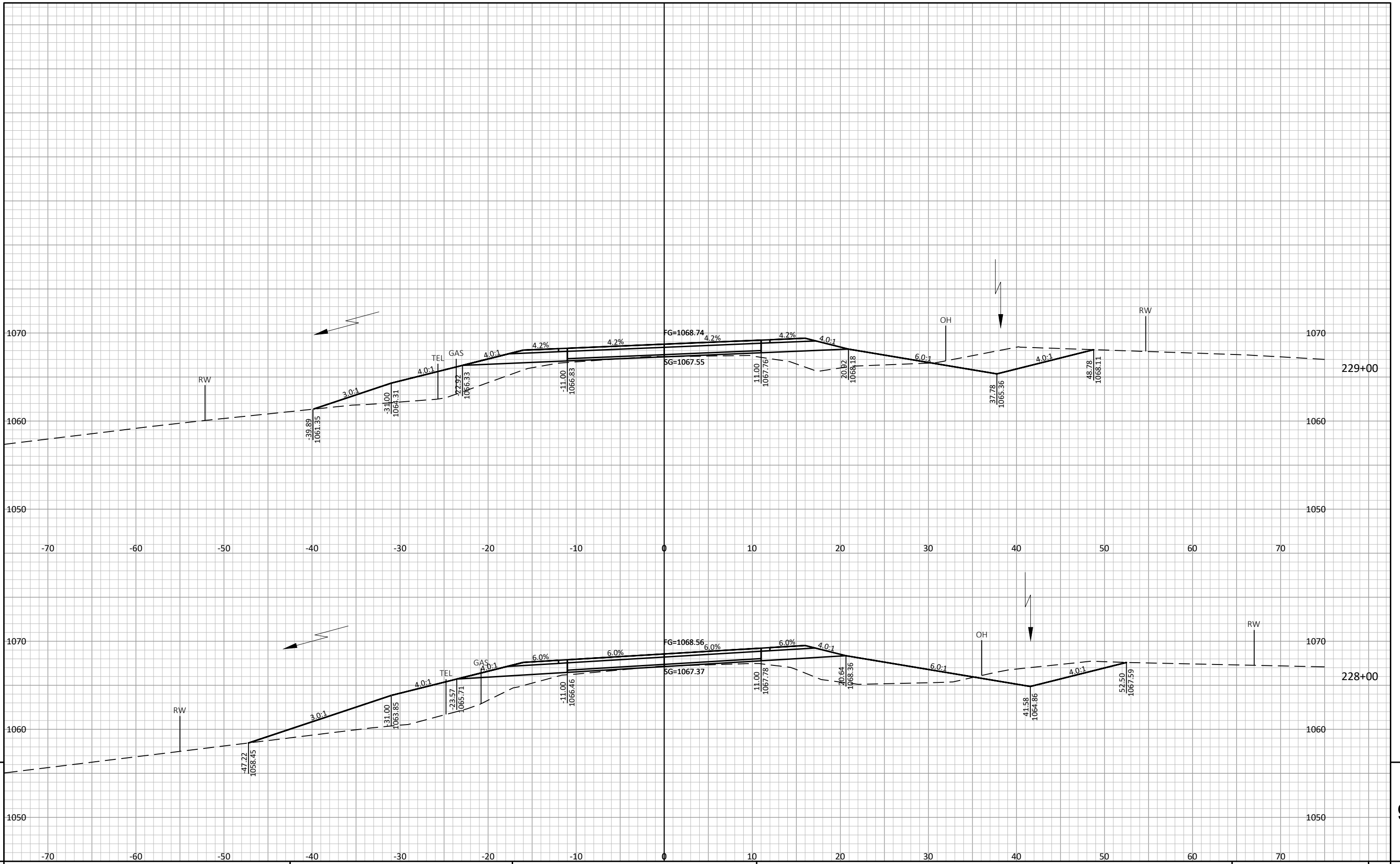
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PROJECT NO: 5034-00-72

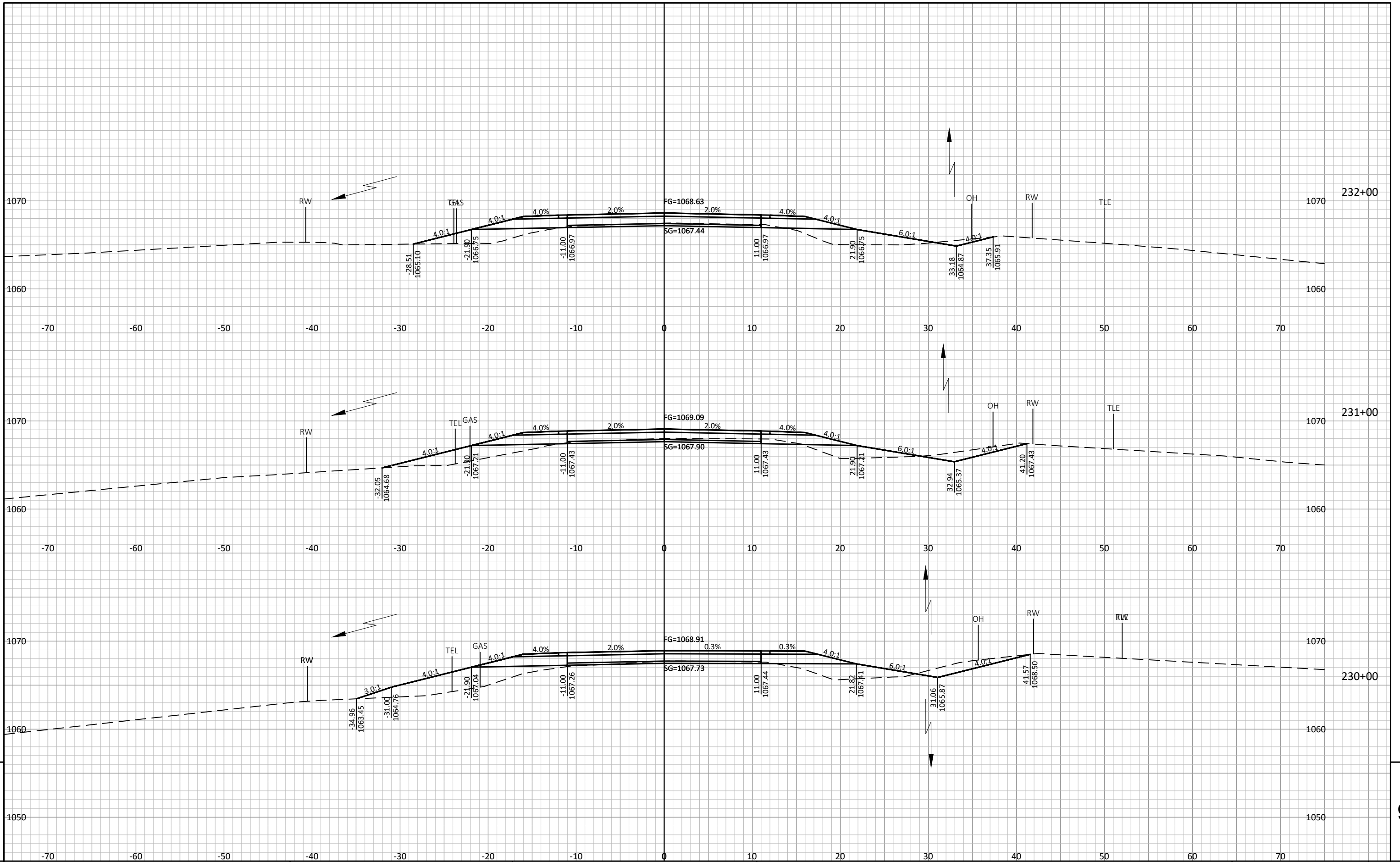
HWY: CTH F

COUNTY: IOWA

CROSS SECTIONS: MAINLINE

SHEET

E



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PROJECT NO: 5034-00-72

HWY: CTH F

COUNTY: IOWA

CROSS SECTIONS: MAINLINE

SHEET

E

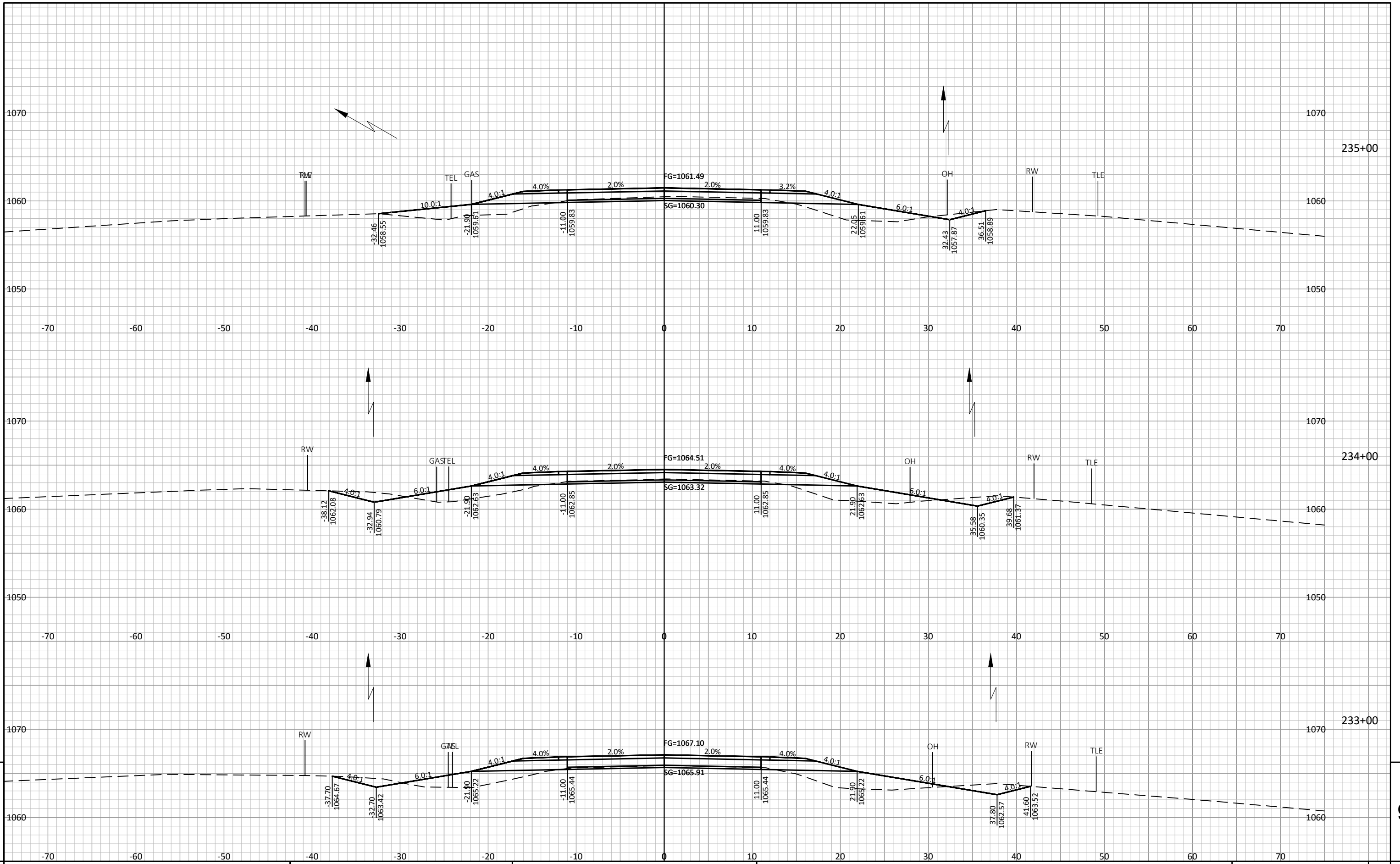
FILE NAME : S:\PROJECTS\K19250 CTH F IOWA COUNTY\DESIGN\CORRIDORS\CORRIDOR_CTH F.DWG

PLOT DATE : 9/8/2021 12:27:02 PM

PLOT BY : JONAH DRAKE

PLOT SCALE : 1" = 1'

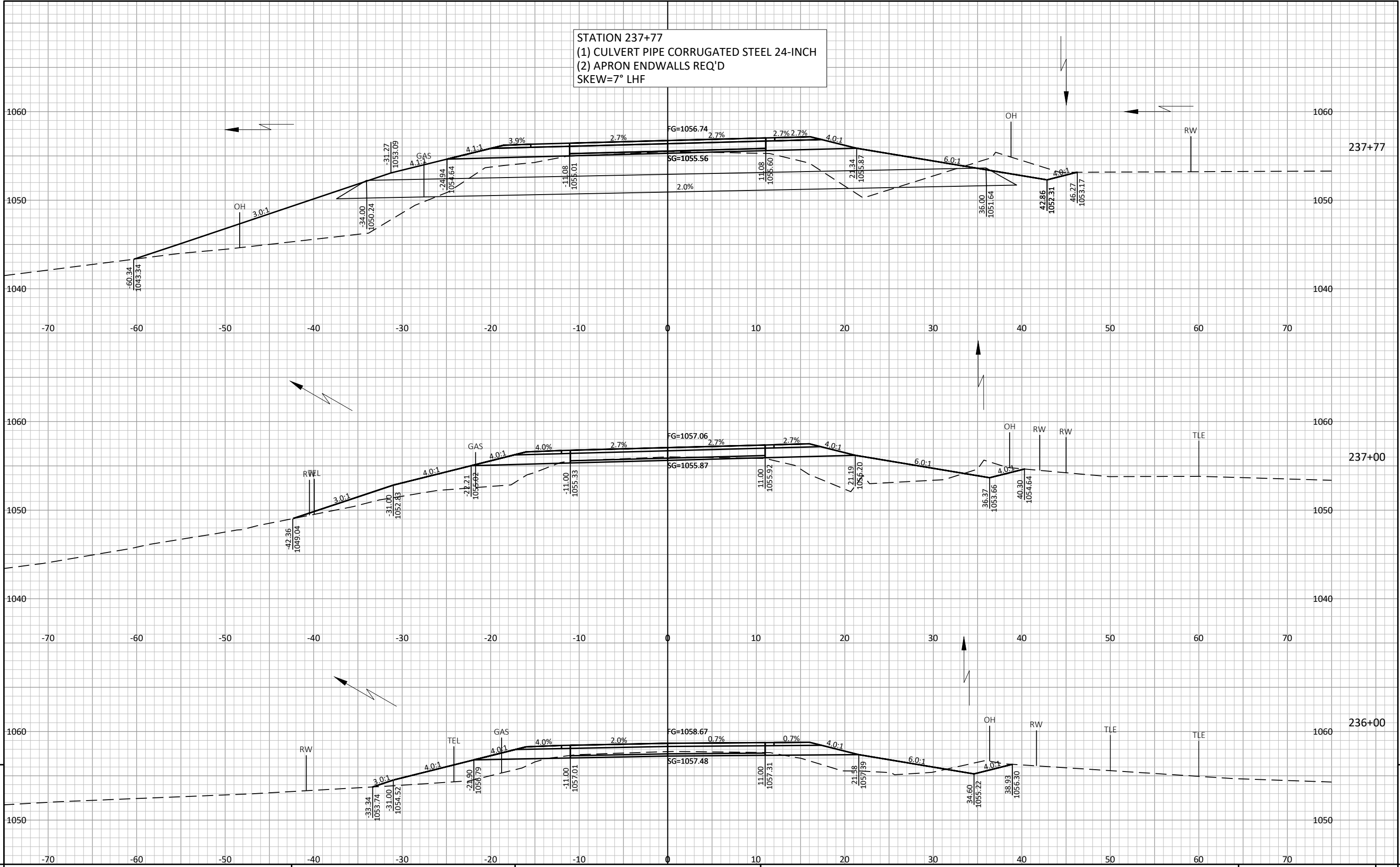
LAYOUT : CTH K 106



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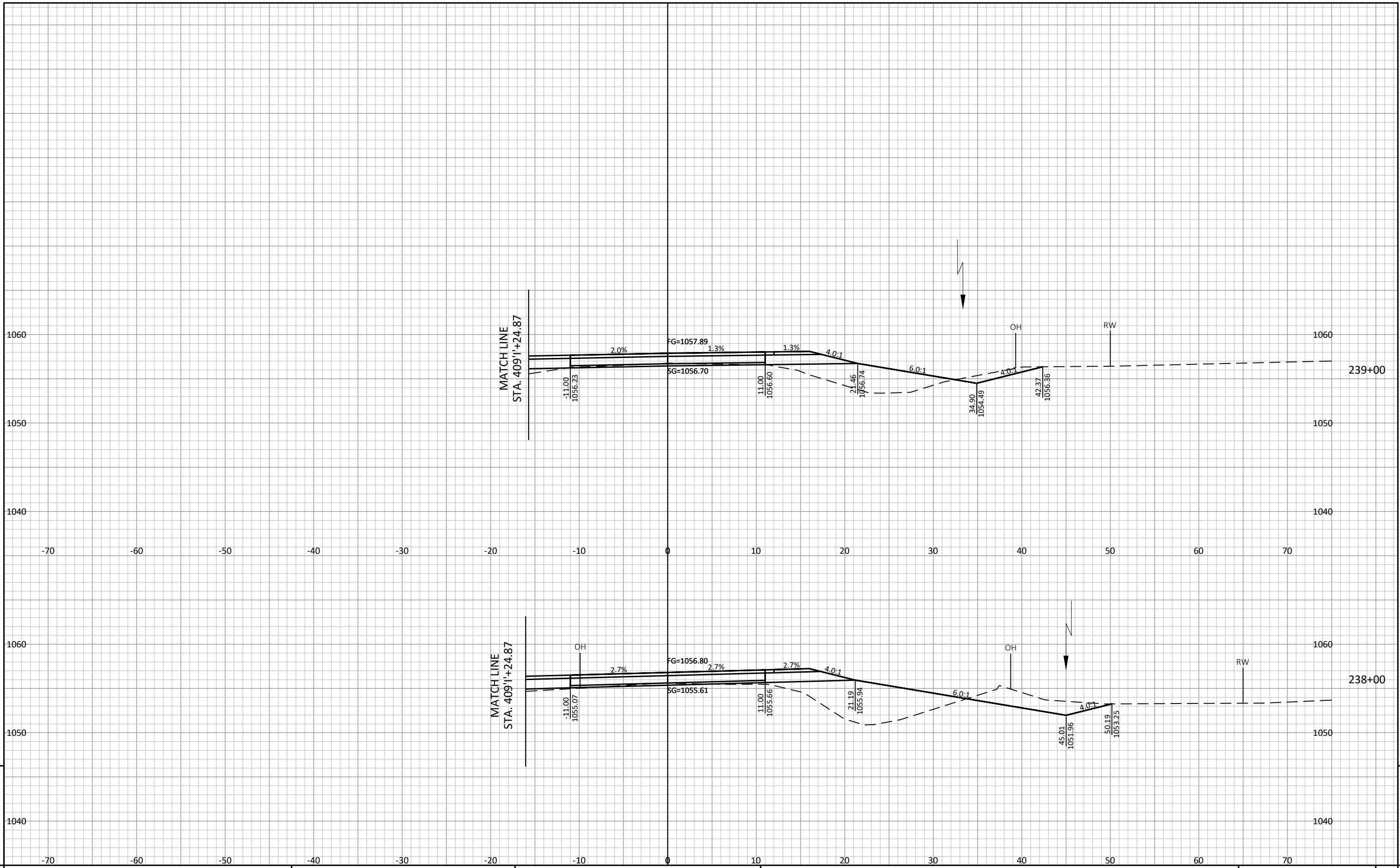
9

STATION 237+77
 (1) CULVERT PIPE CORRUGATED STEEL 24-INCH
 (2) APRON ENDWALLS REQ'D
 SKEW=7° LHF



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PROJECT NO: 5034-00-72

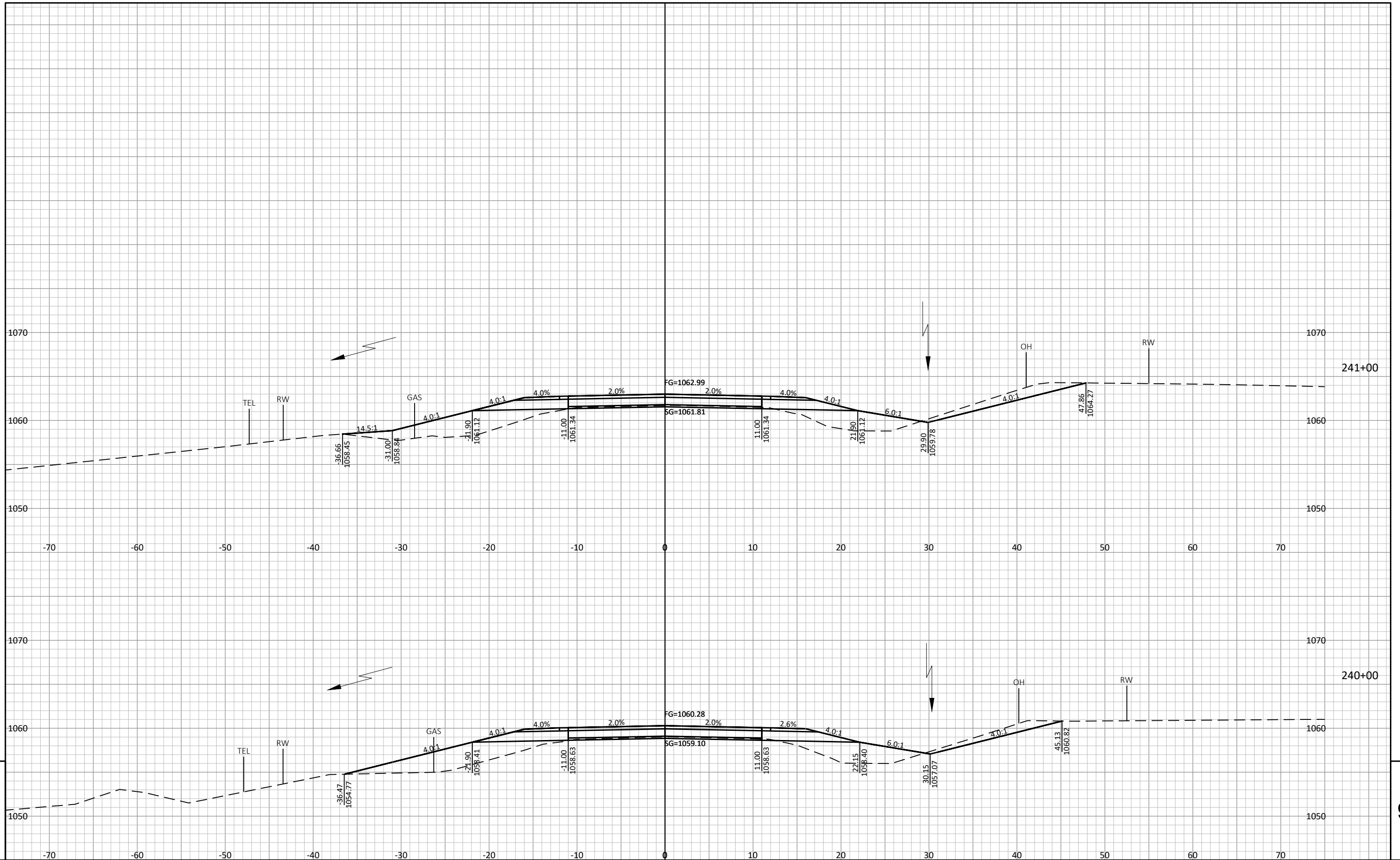
HWY: CTH F

COUNTY: IOWA

CROSS SECTIONS: MAINLINE

SHEET

E



PROJECT NO: 5034-00-72

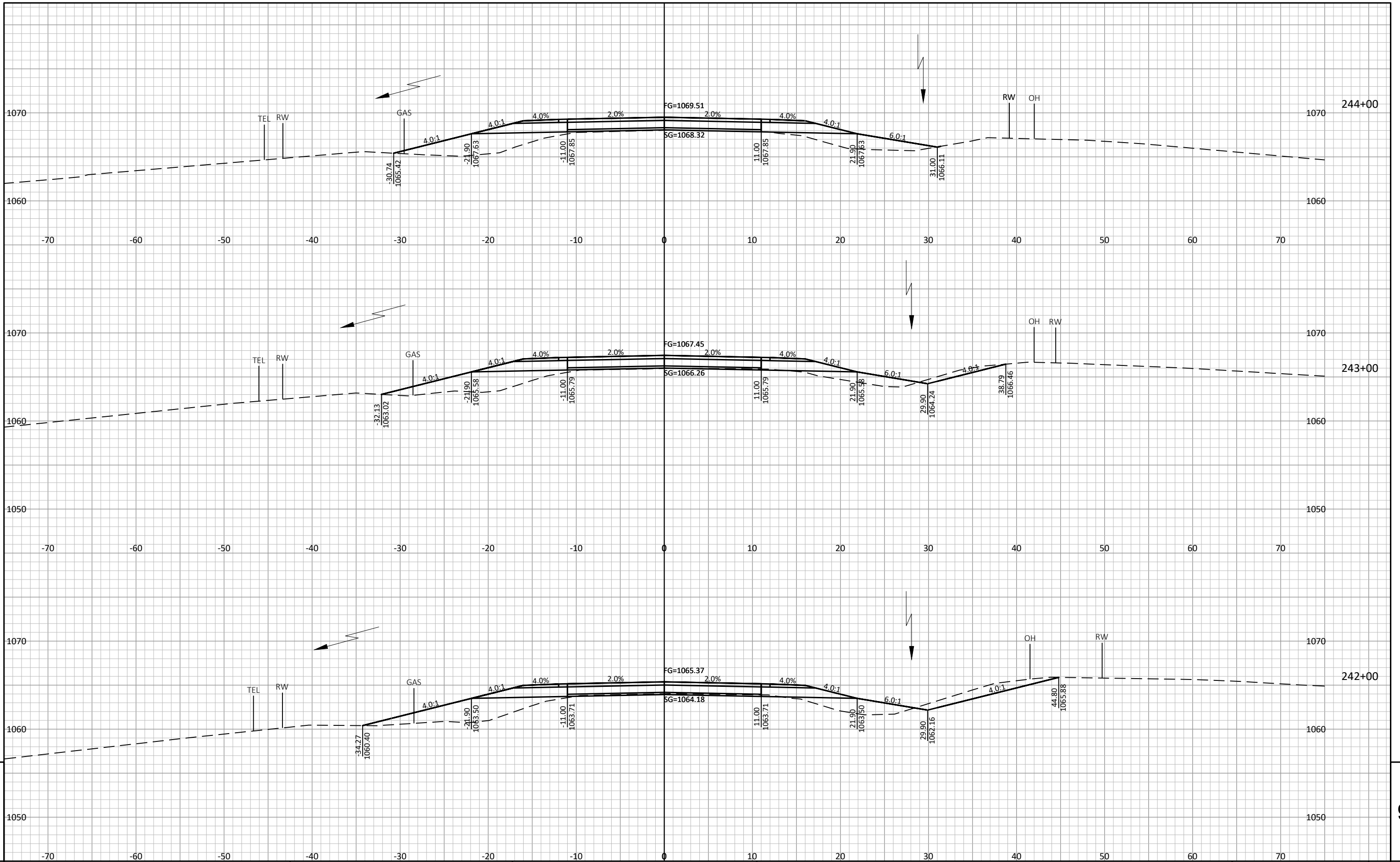
HWY: CTH F

COUNTY: IOWA

CROSS SECTIONS: MAINLINE

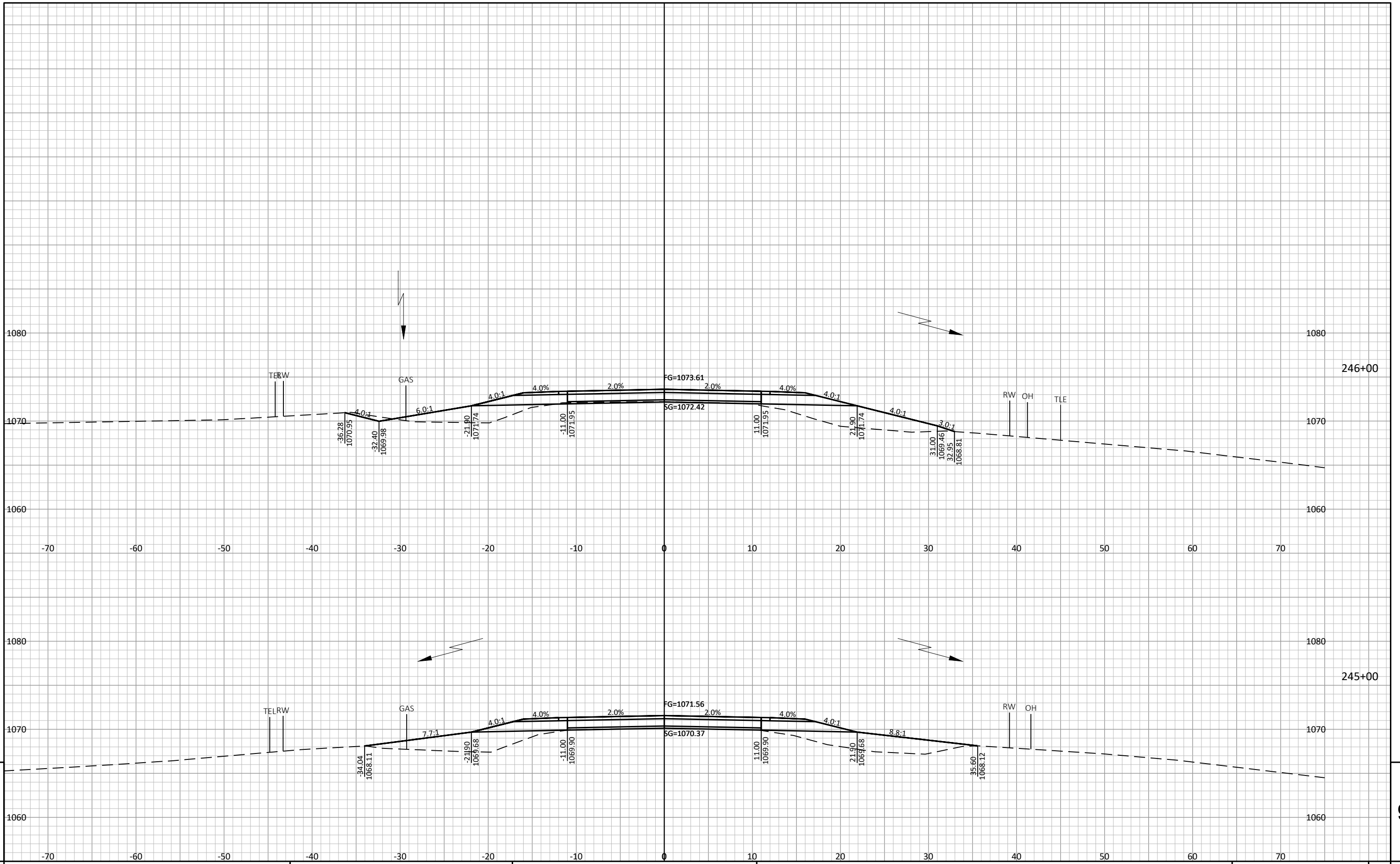
SHEET

E



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PROJECT NO: 5034-00-72

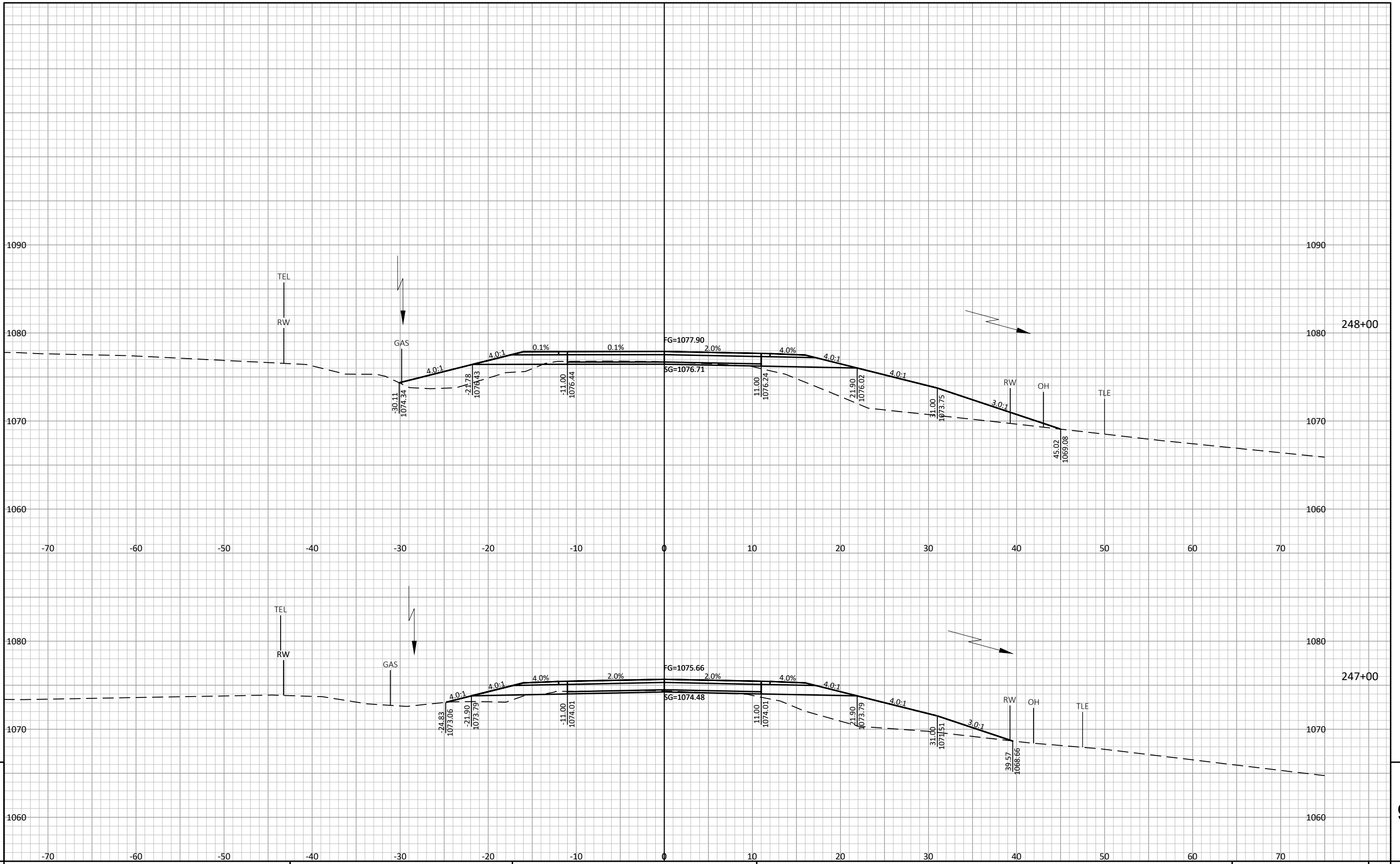
HWY: CTH F

COUNTY: IOWA

CROSS SECTIONS: MAINLINE

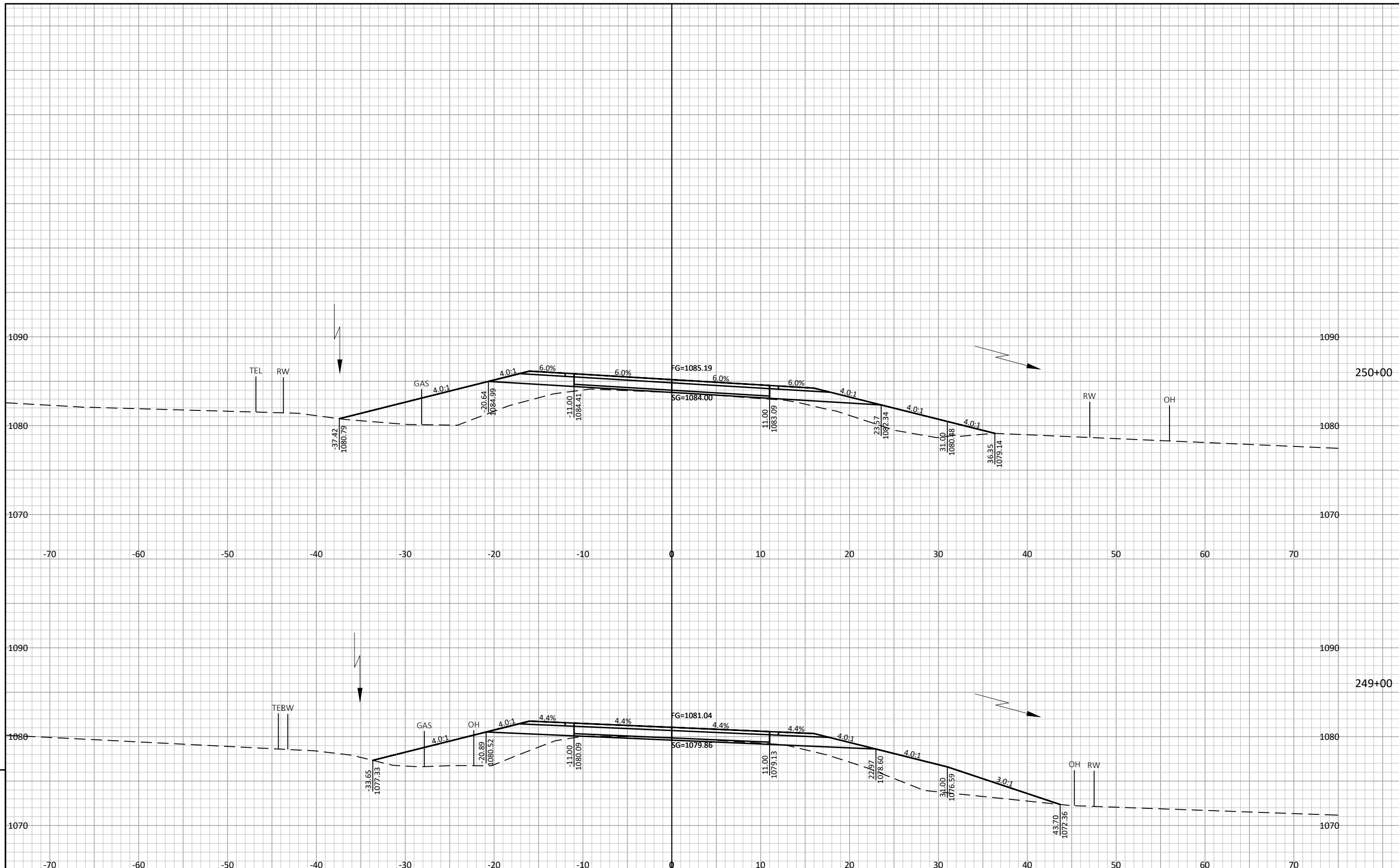
SHEET

E



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PROJECT NO: 5034-00-72

HWY: CTH F

COUNTY: IOWA

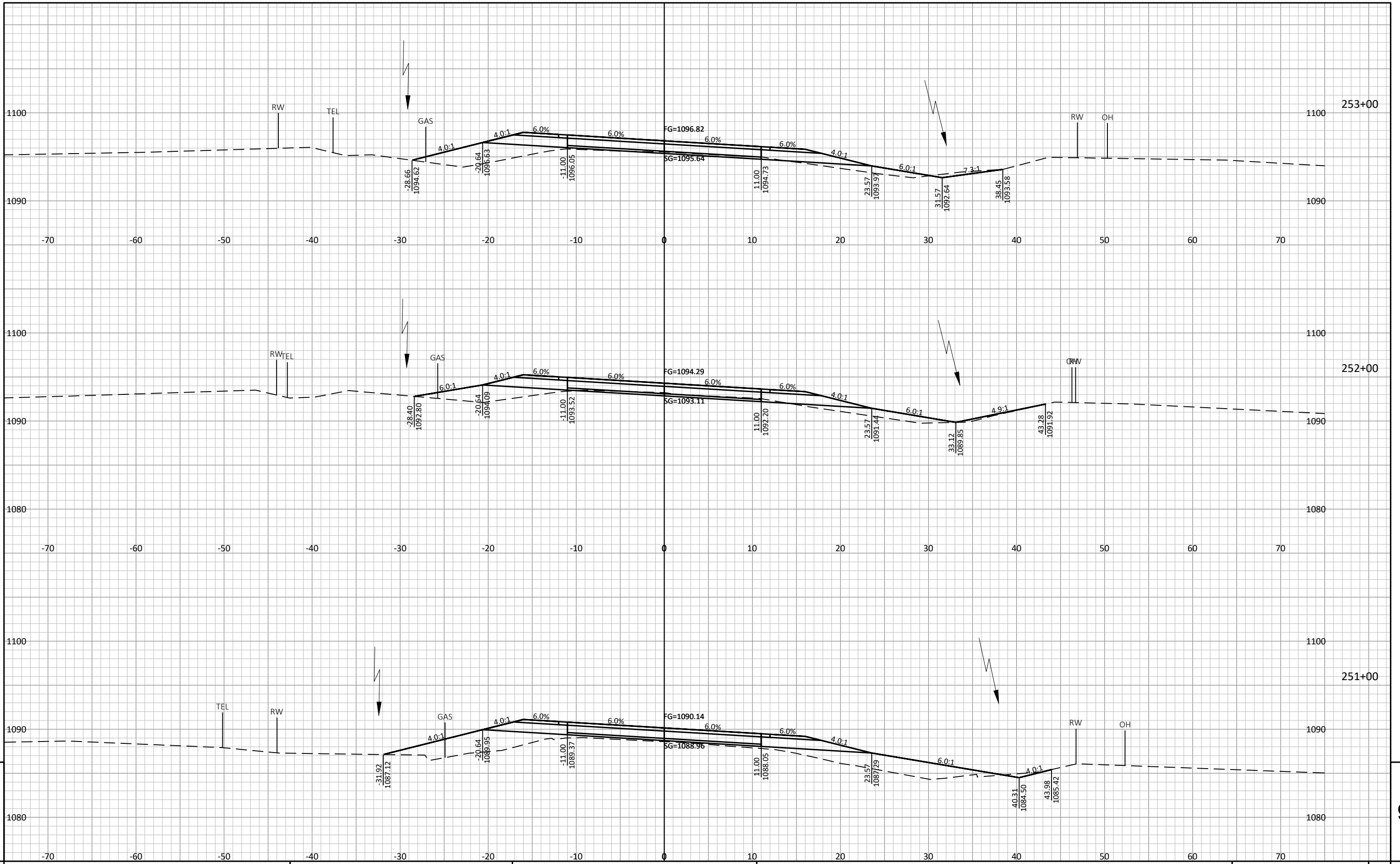
CROSS SECTIONS: MAINLINE

SHEET

E

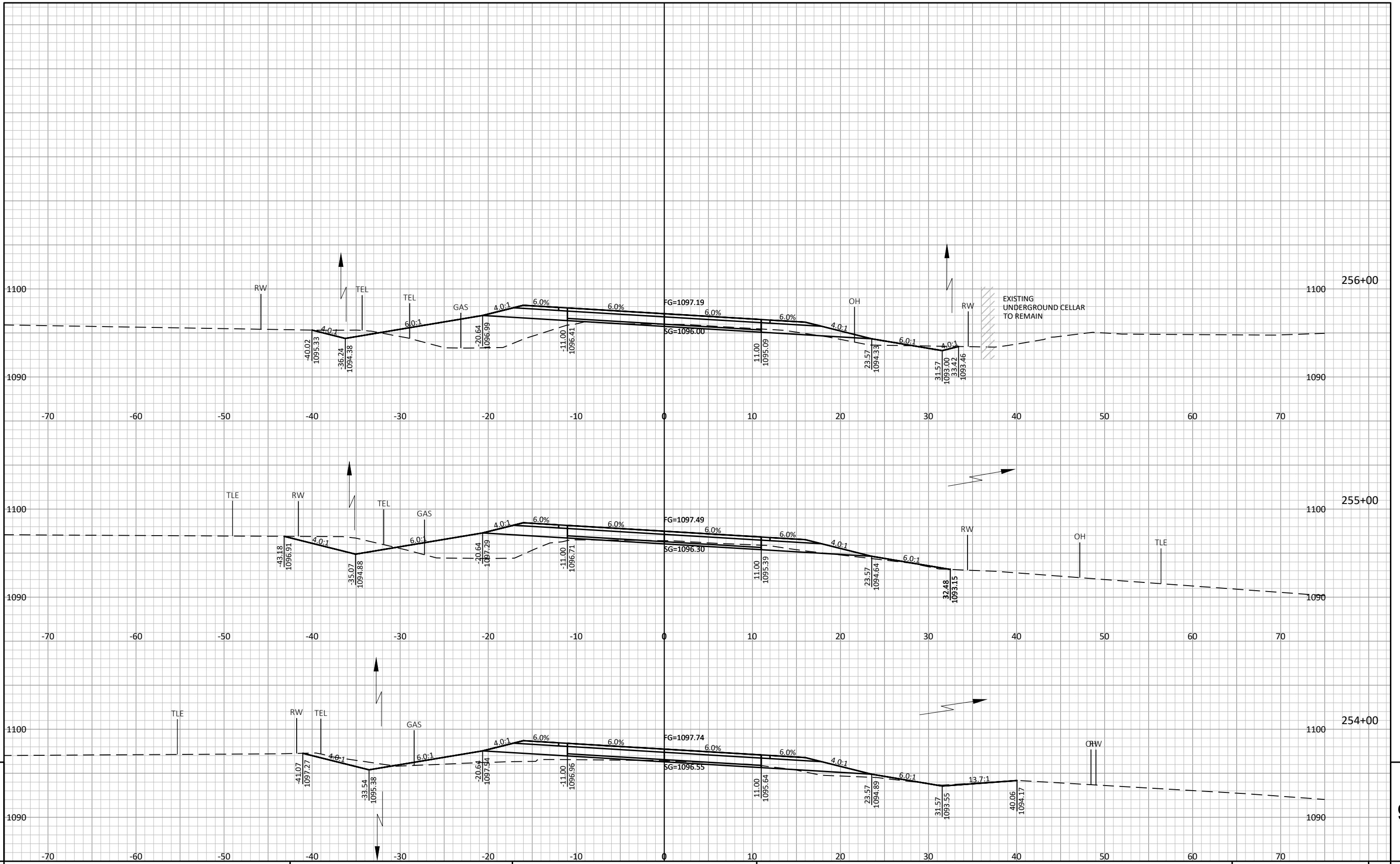
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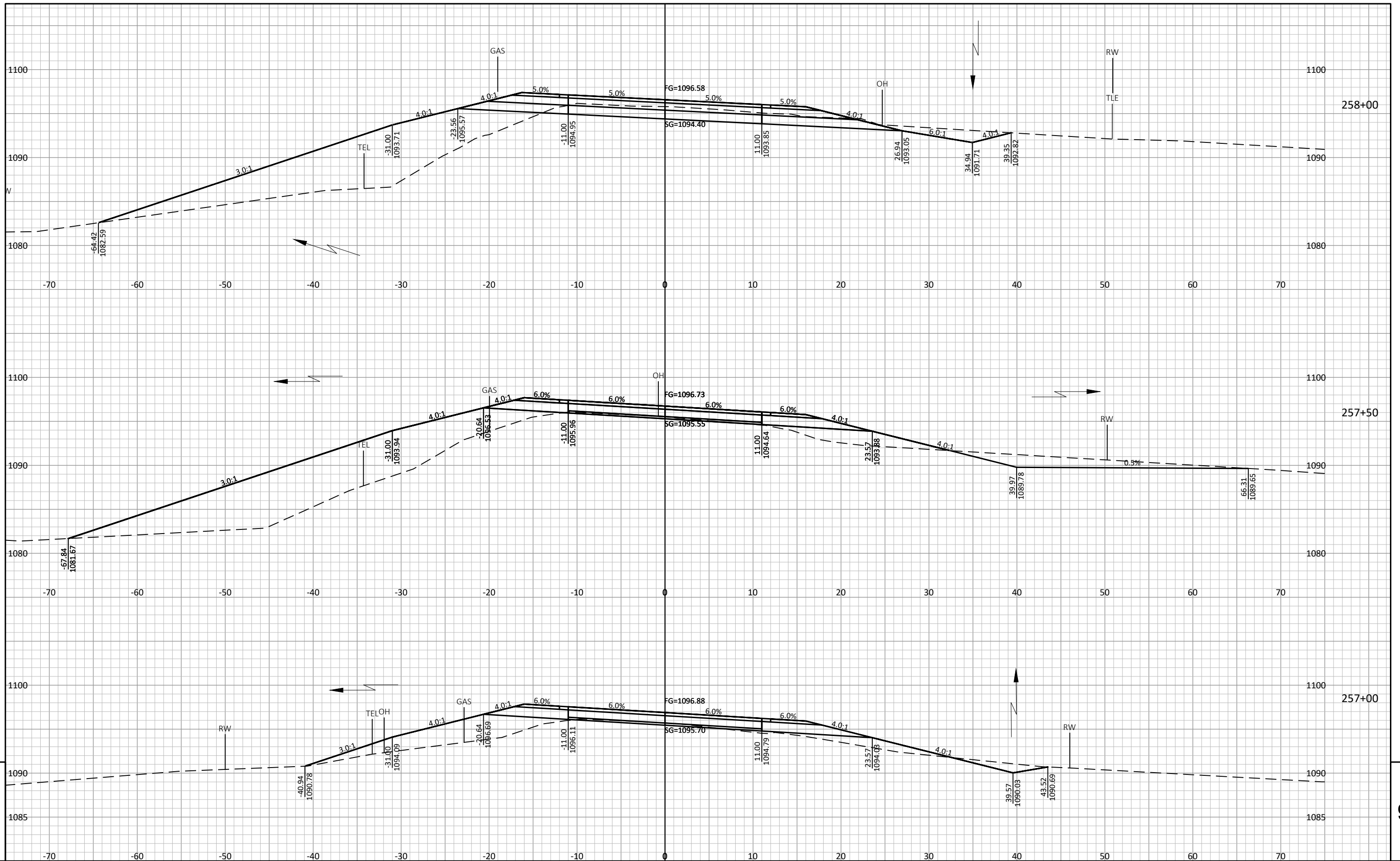
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PROJECT NO: 5034-00-72

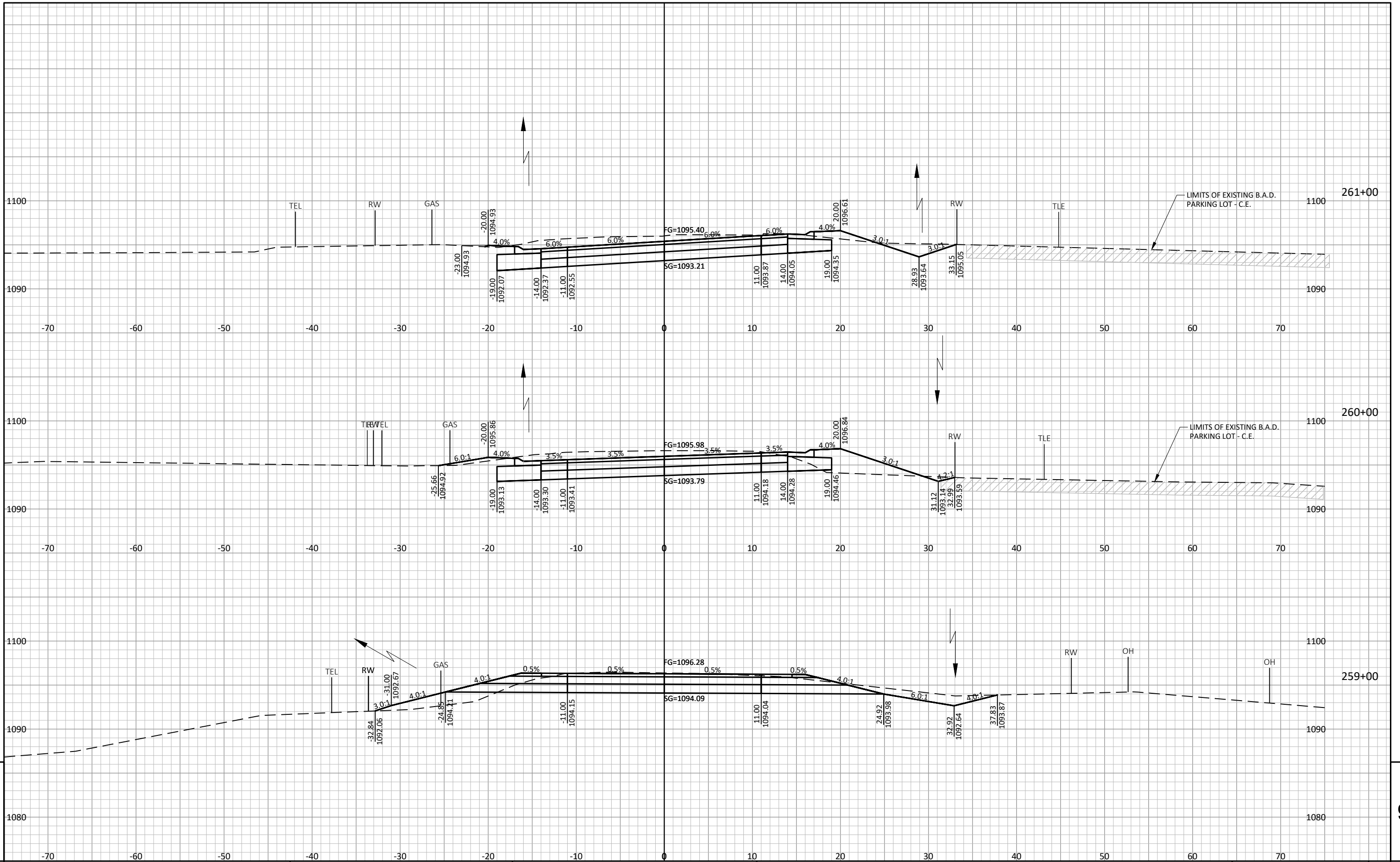
HWY: CTH F

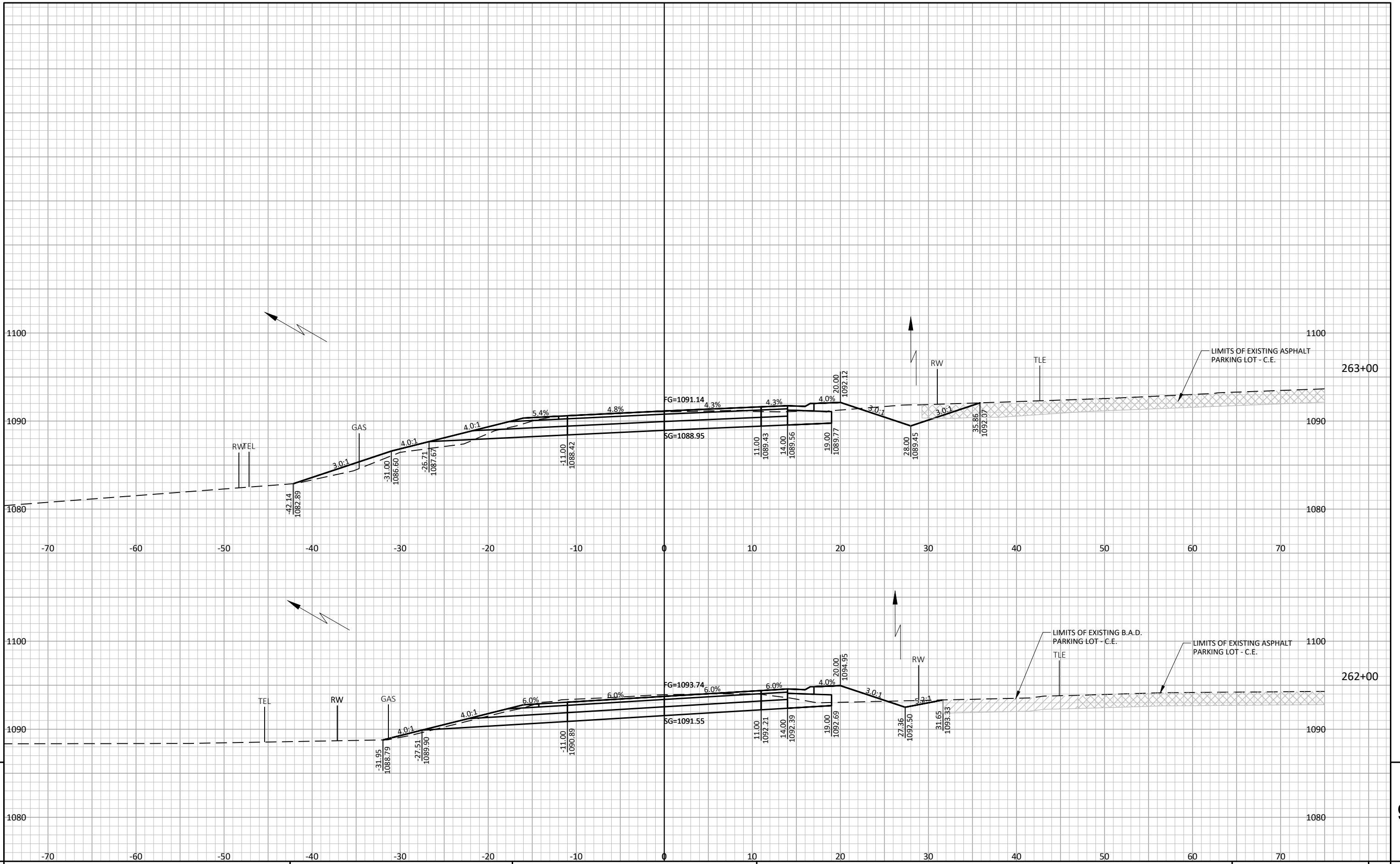
COUNTY: IOWA

CROSS SECTIONS: MAINLINE

SHEET

E





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PROJECT NO: 5034-00-72

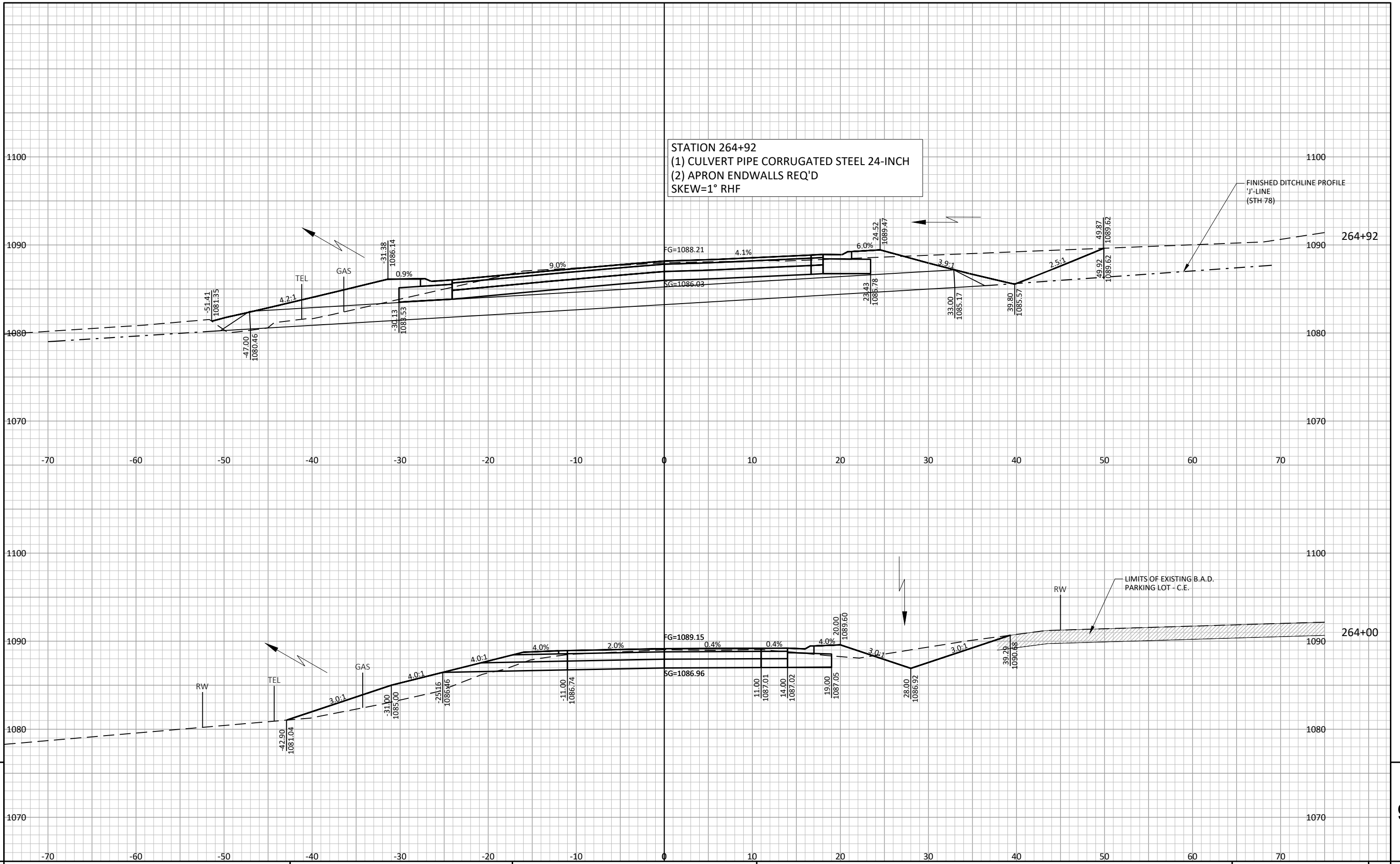
HWY: CTH F

COUNTY: IOWA

CROSS SECTIONS: MAINLINE

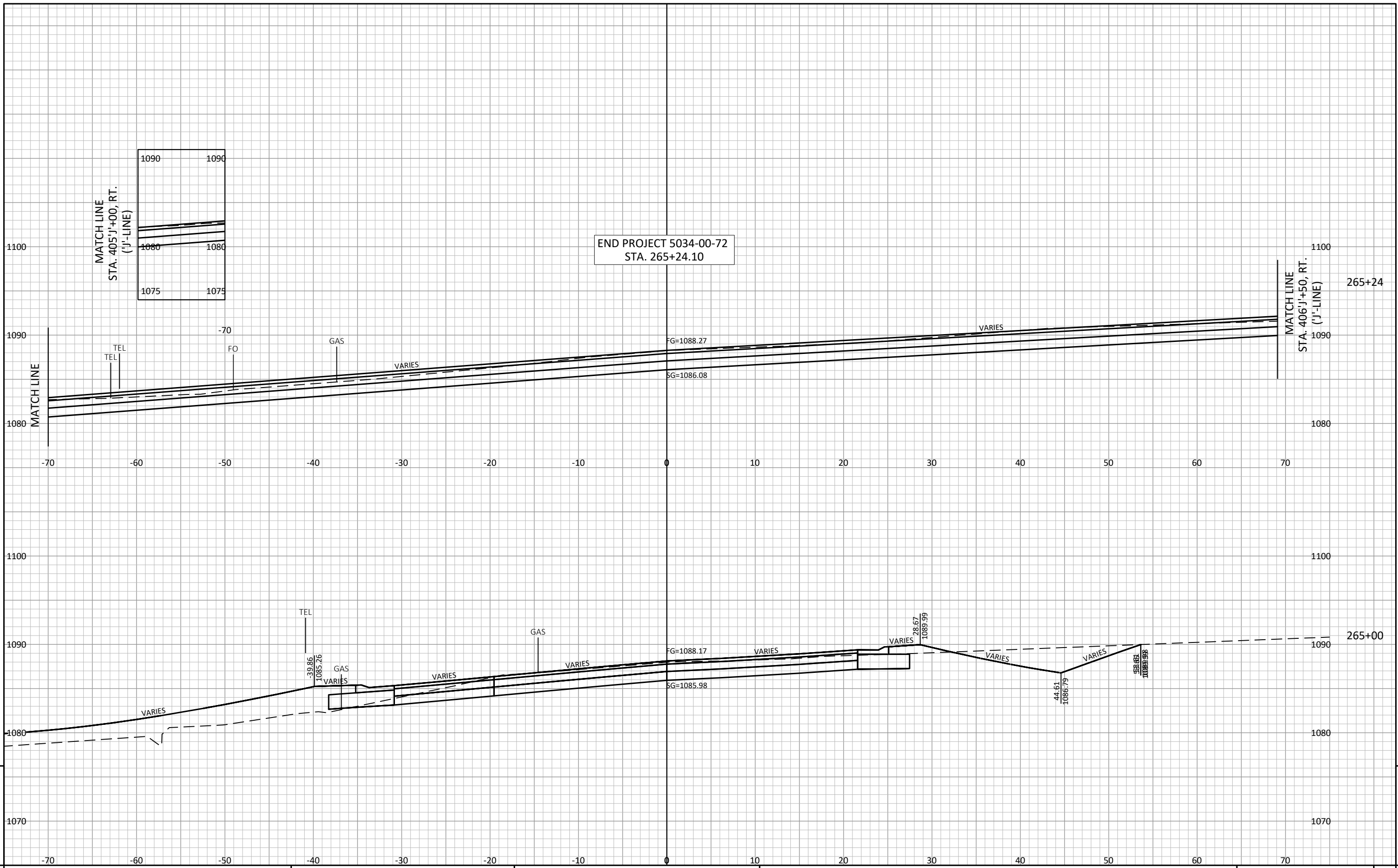
SHEET

E



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PROJECT NO: 5034-00-72

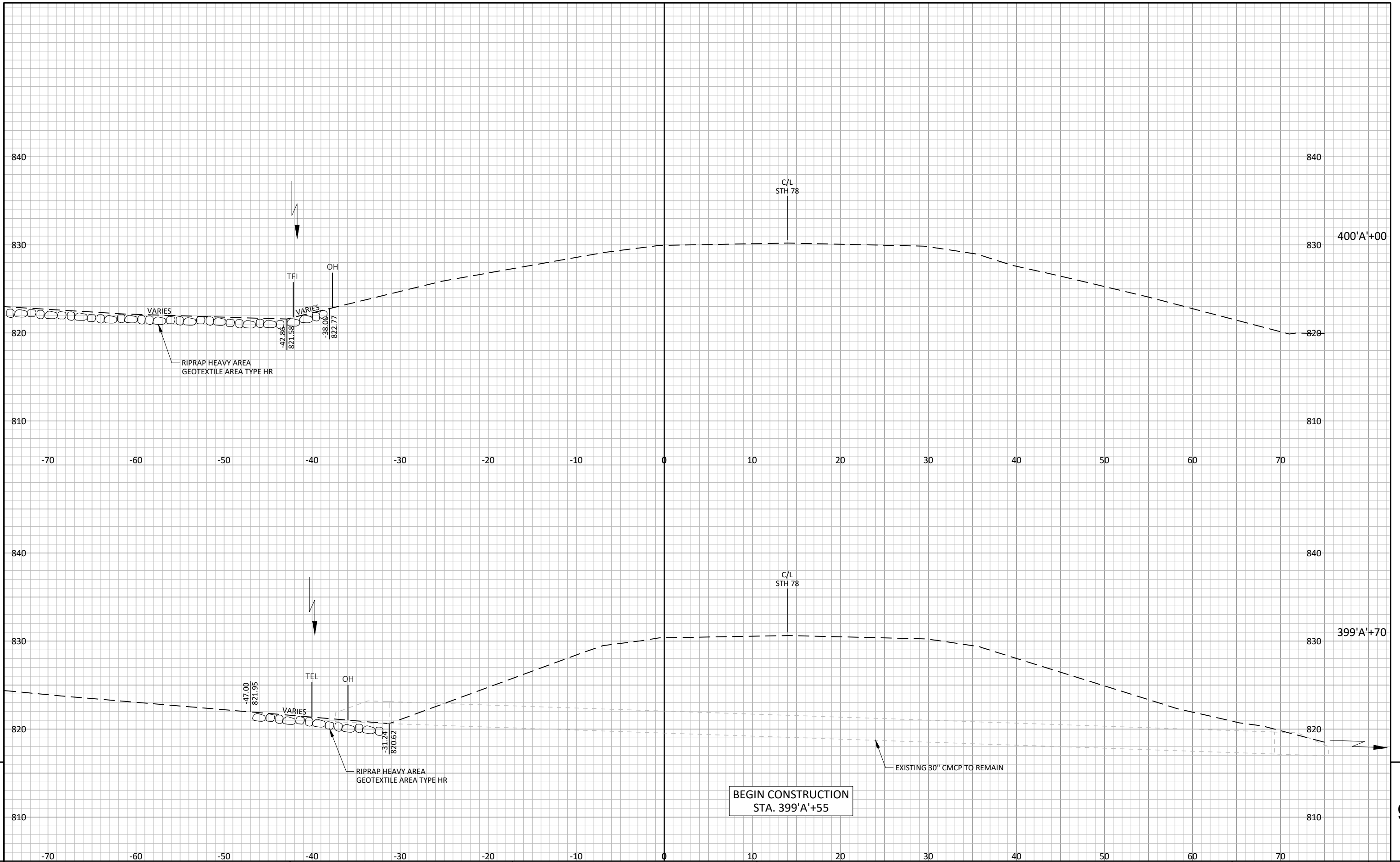
HWY: CTH F

COUNTY: IOWA

CROSS SECTIONS: MAINLINE

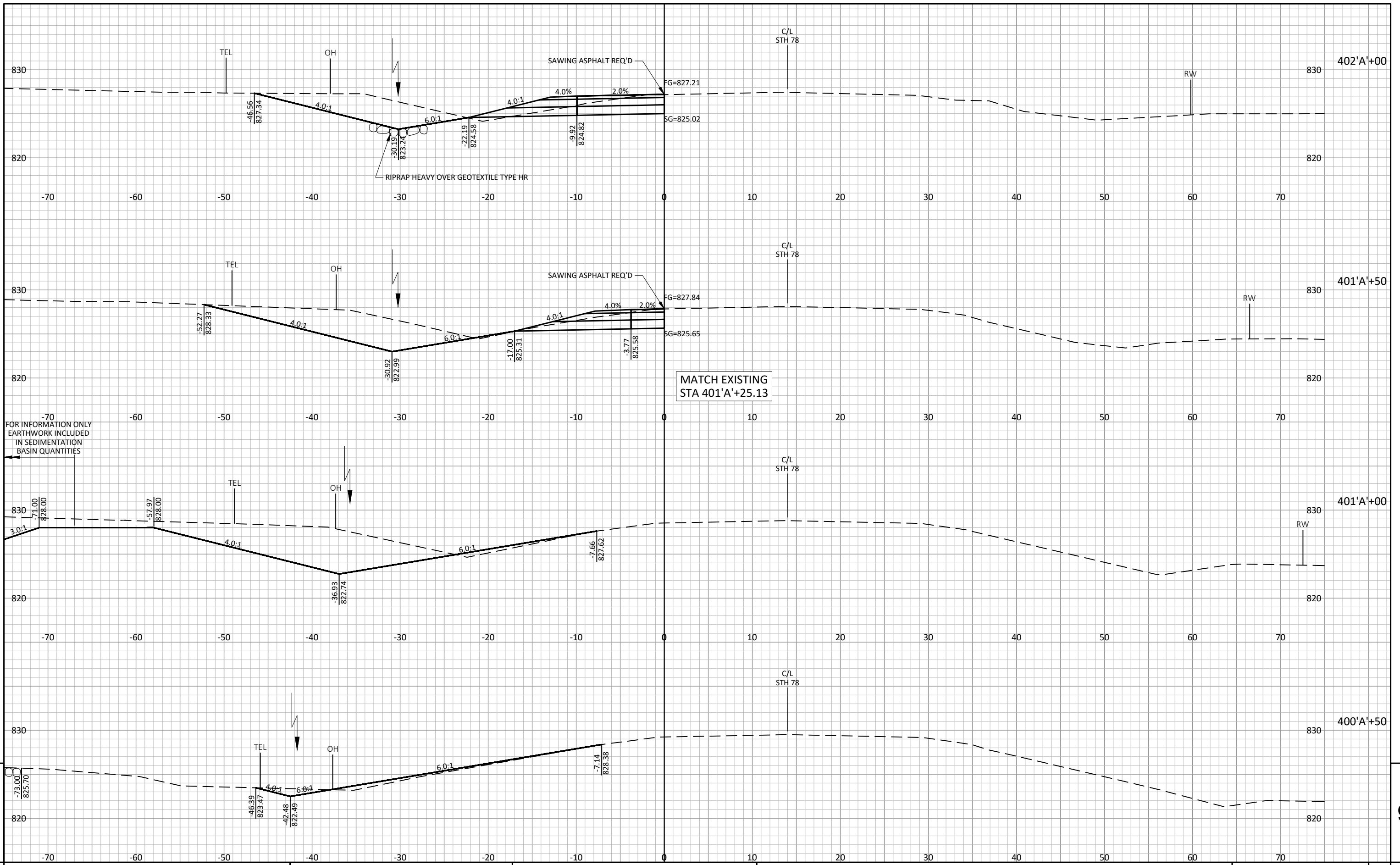
SHEET

E



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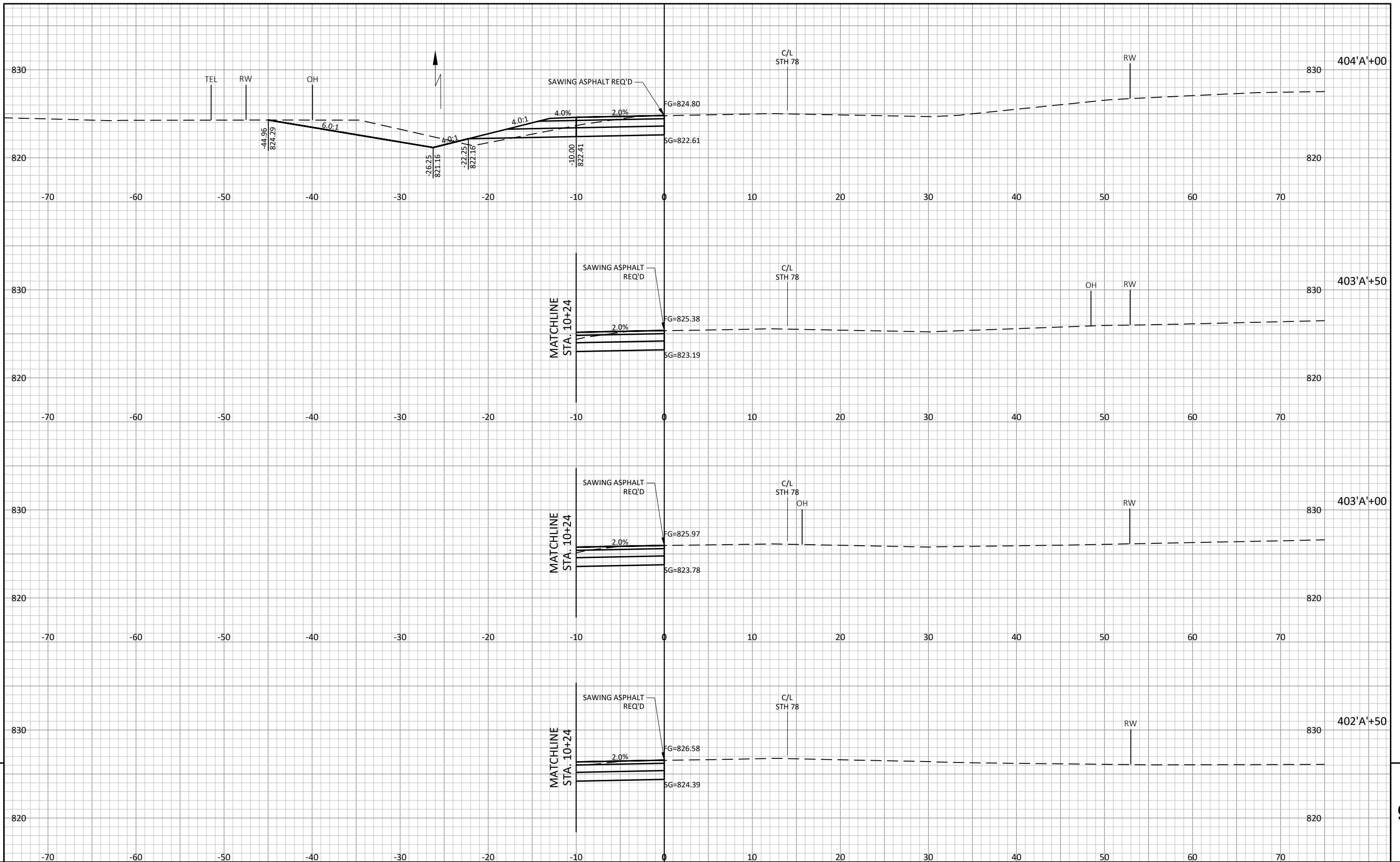


FOR INFORMATION ONLY
EARTHWORK INCLUDED
IN SEDIMENTATION
BASIN QUANTITIES

MATCH EXISTING
STA 401'A'+25.13

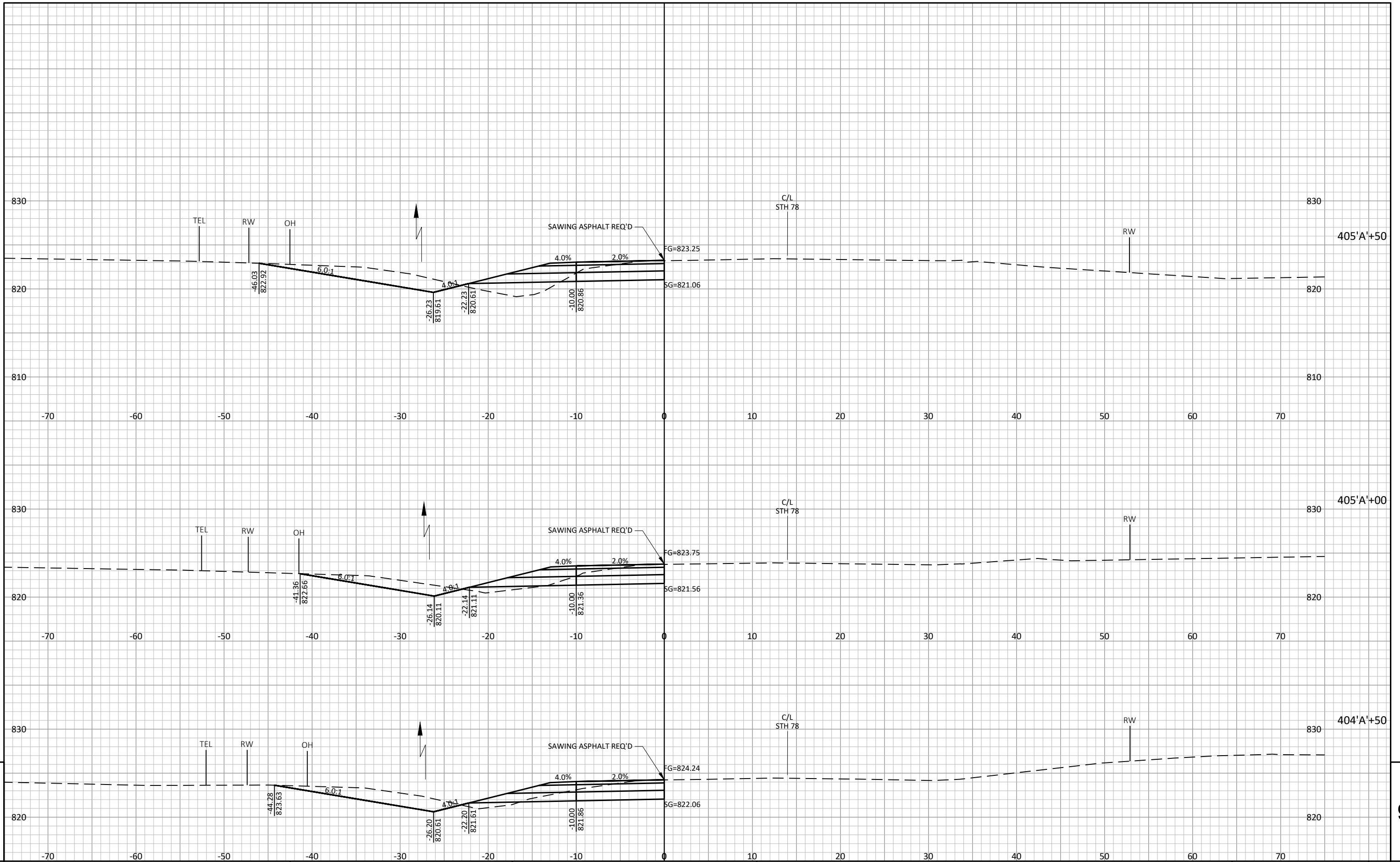
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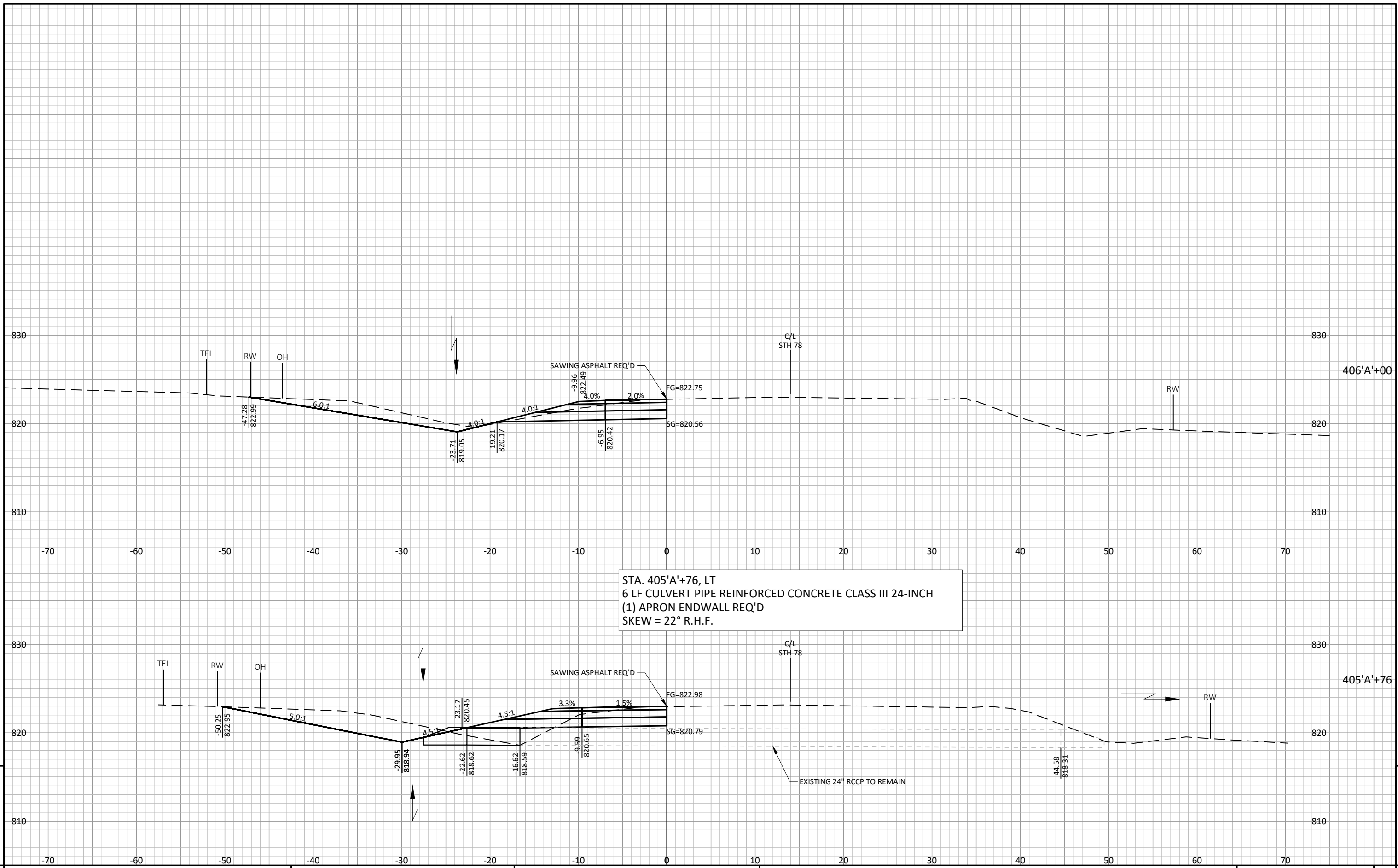
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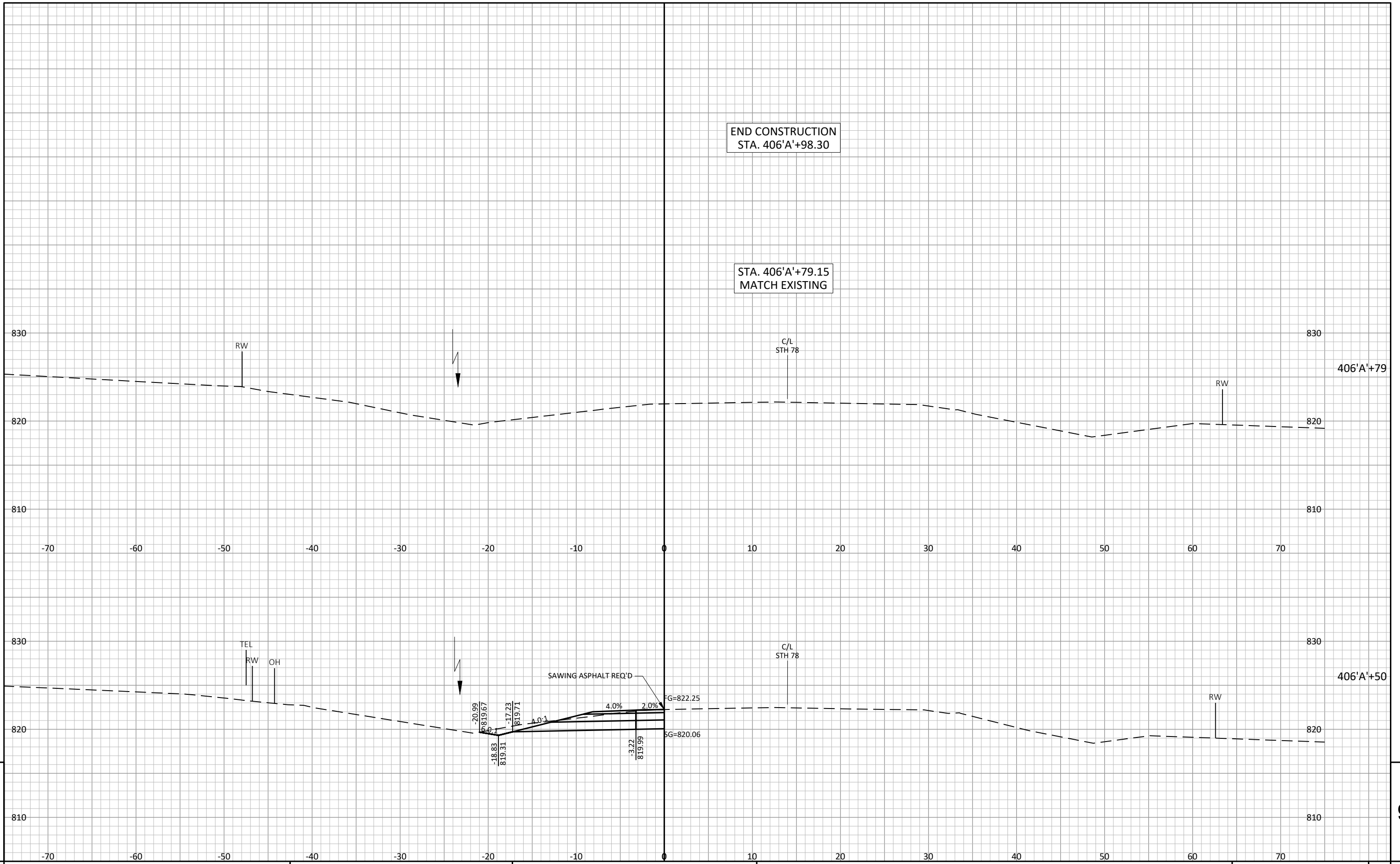




STA. 405'A'+76, LT
 6 LF CULVERT PIPE REINFORCED CONCRETE CLASS III 24-INCH
 (1) APRON ENDWALL REQ'D
 SKEW = 22° R.H.F.

9

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END CONSTRUCTION
STA. 406'A'+98.30

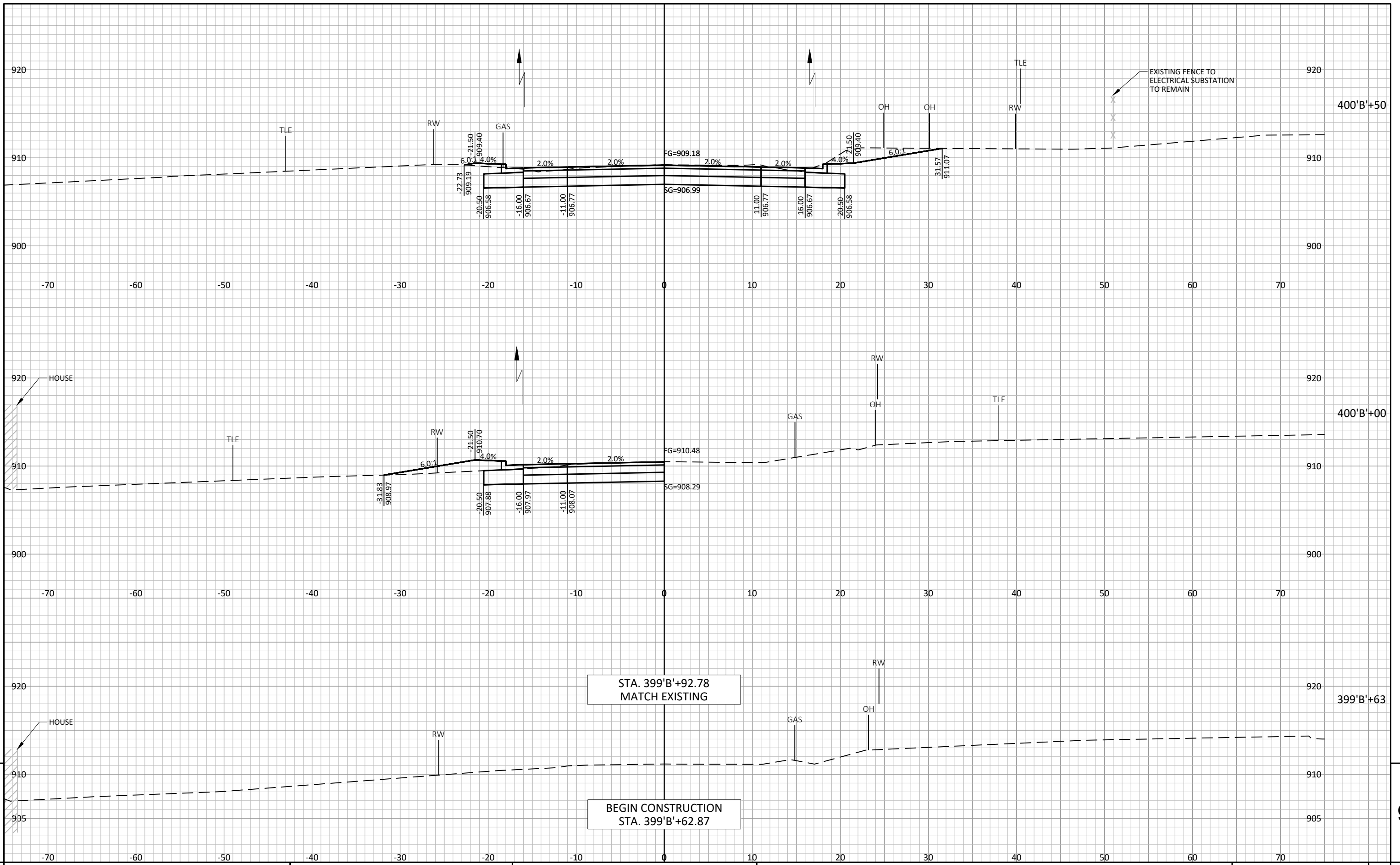
STA. 406'A'+79.15
MATCH EXISTING

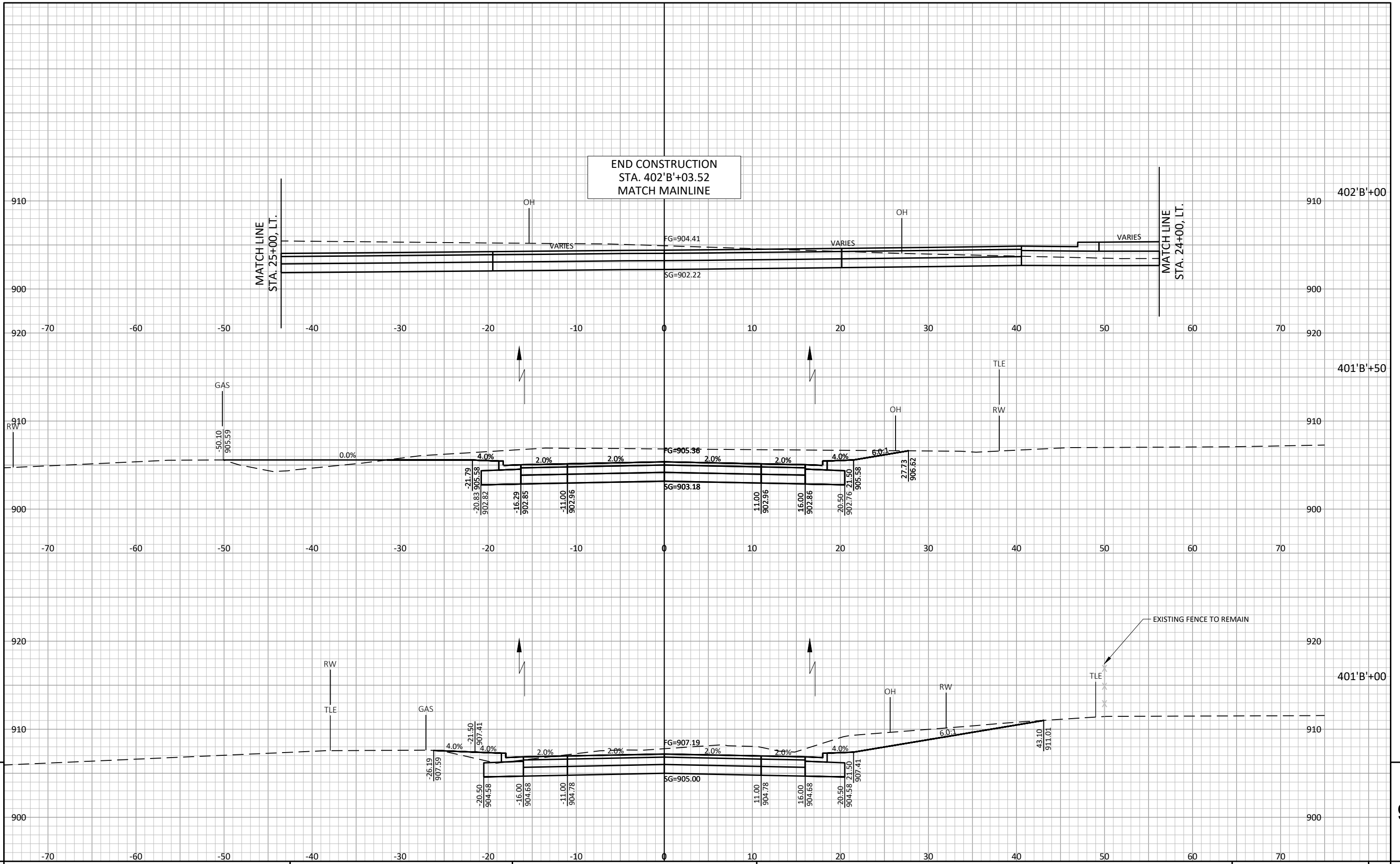
406'A'+79

406'A'+50

9

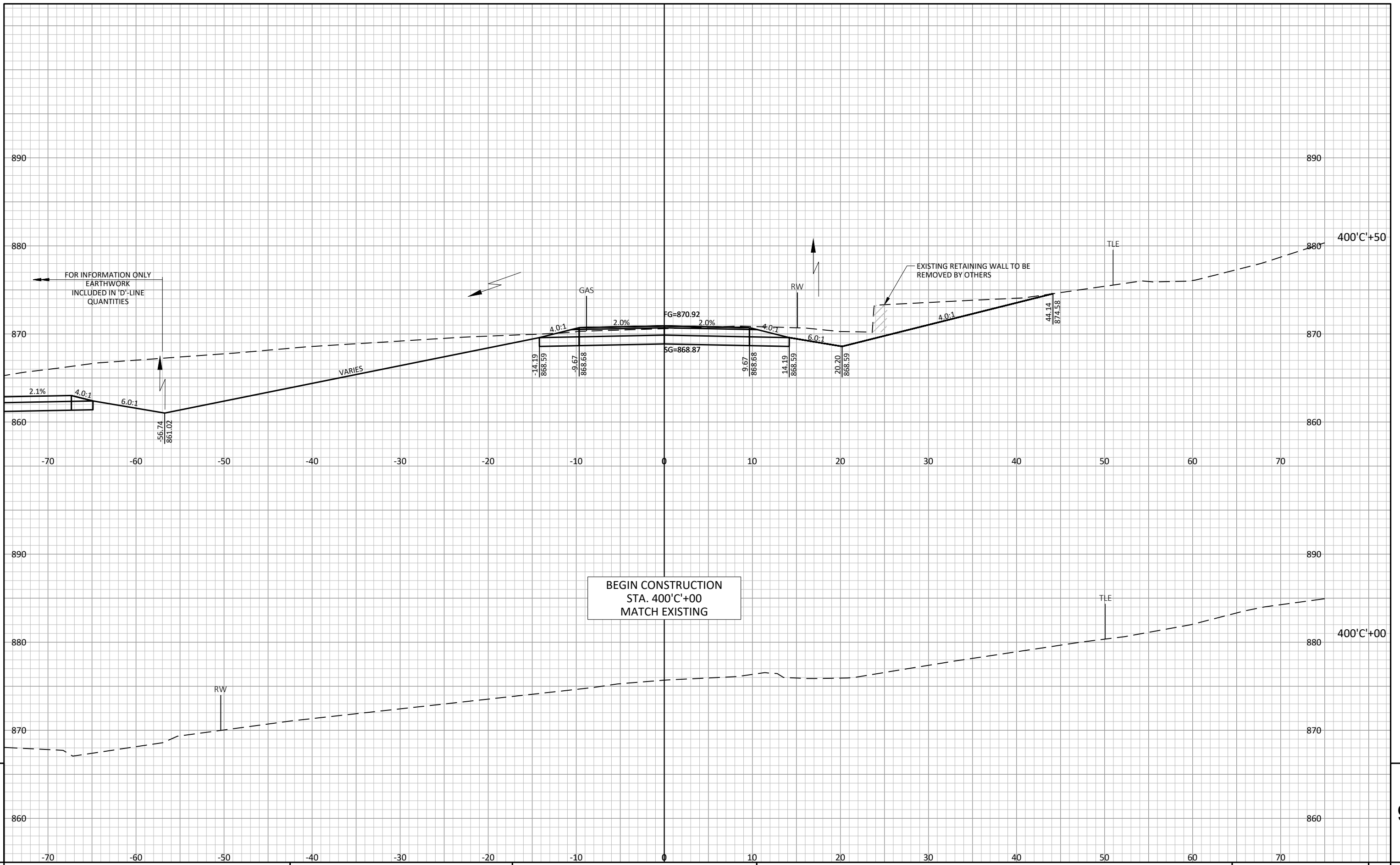
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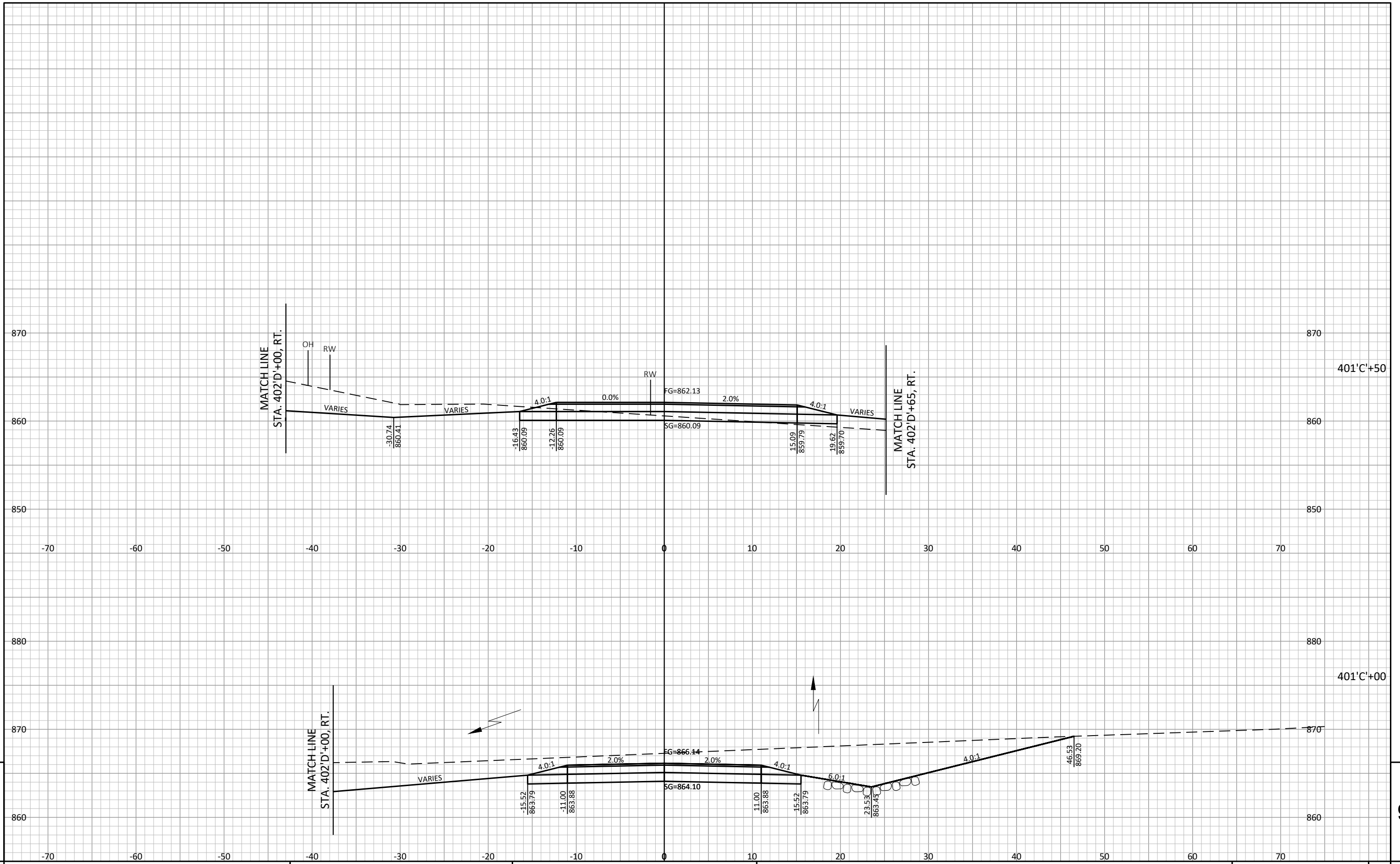
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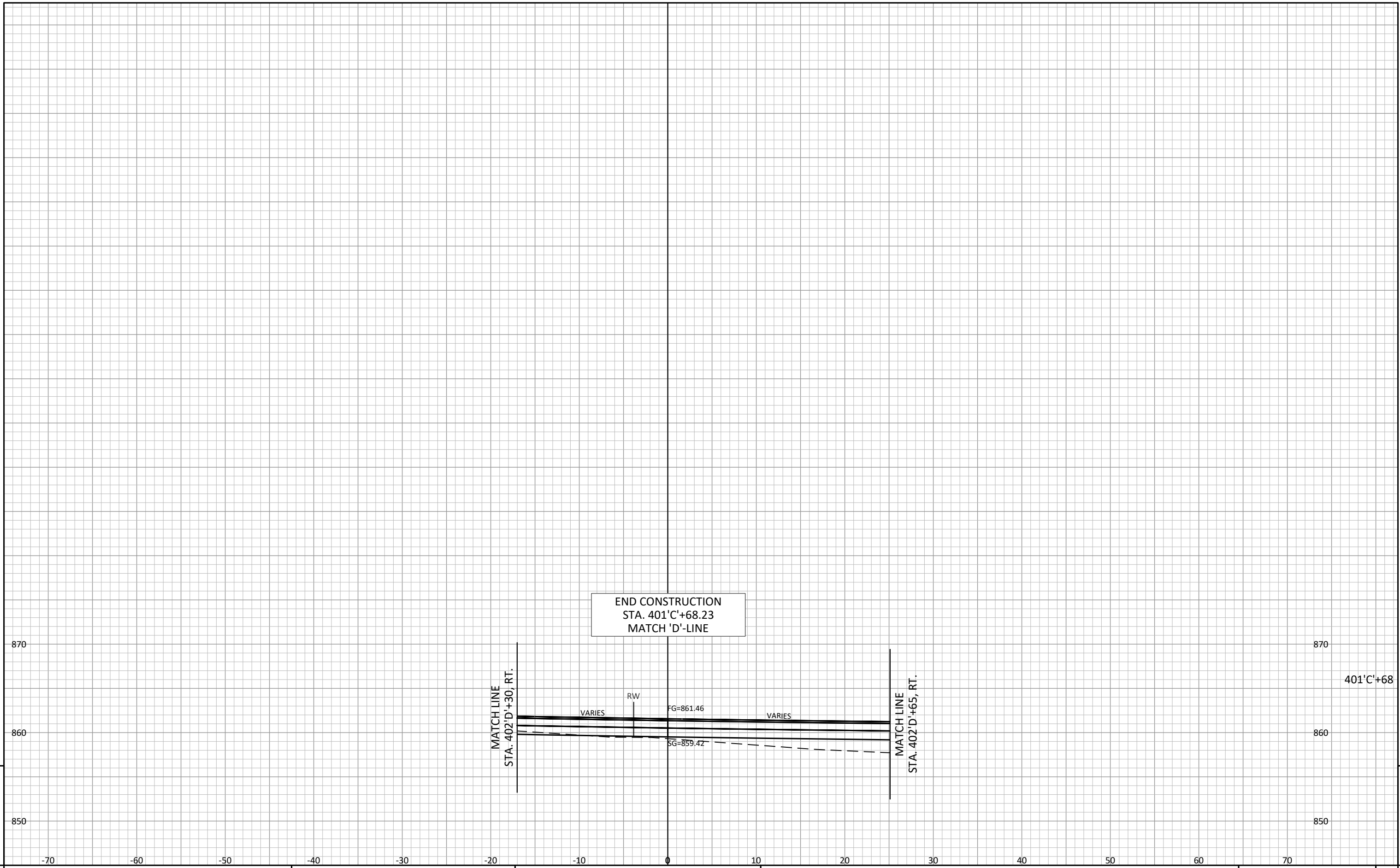
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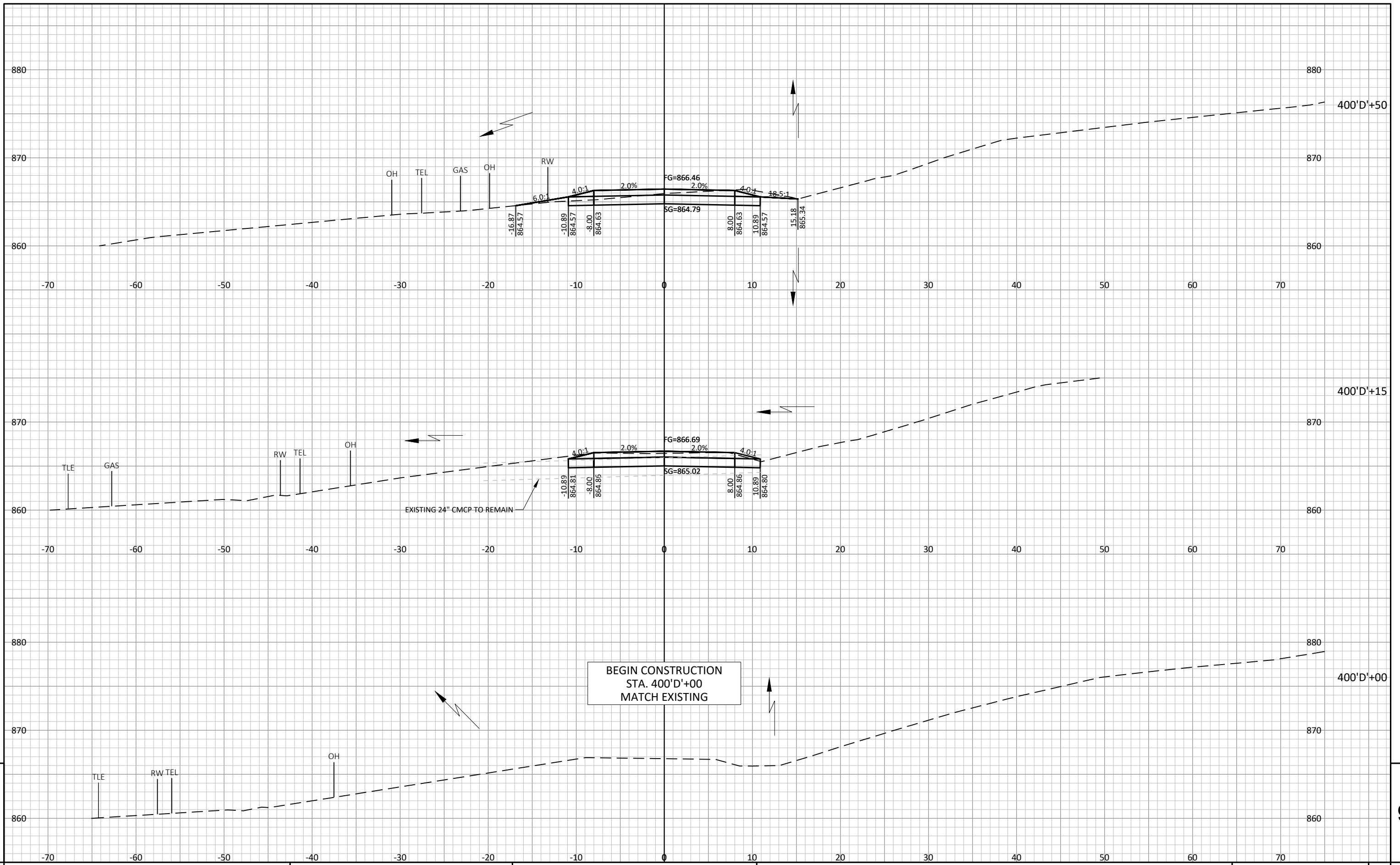
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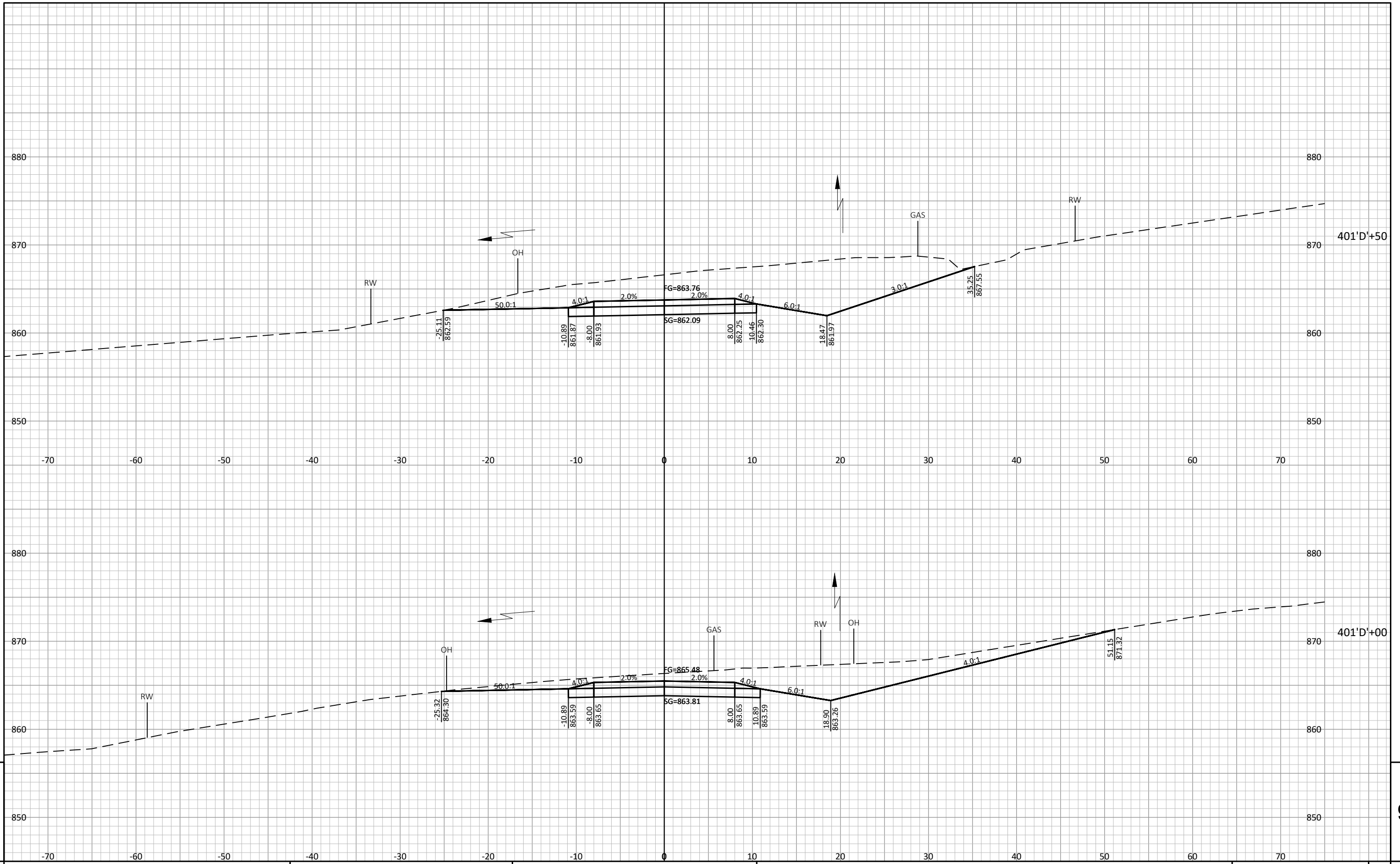




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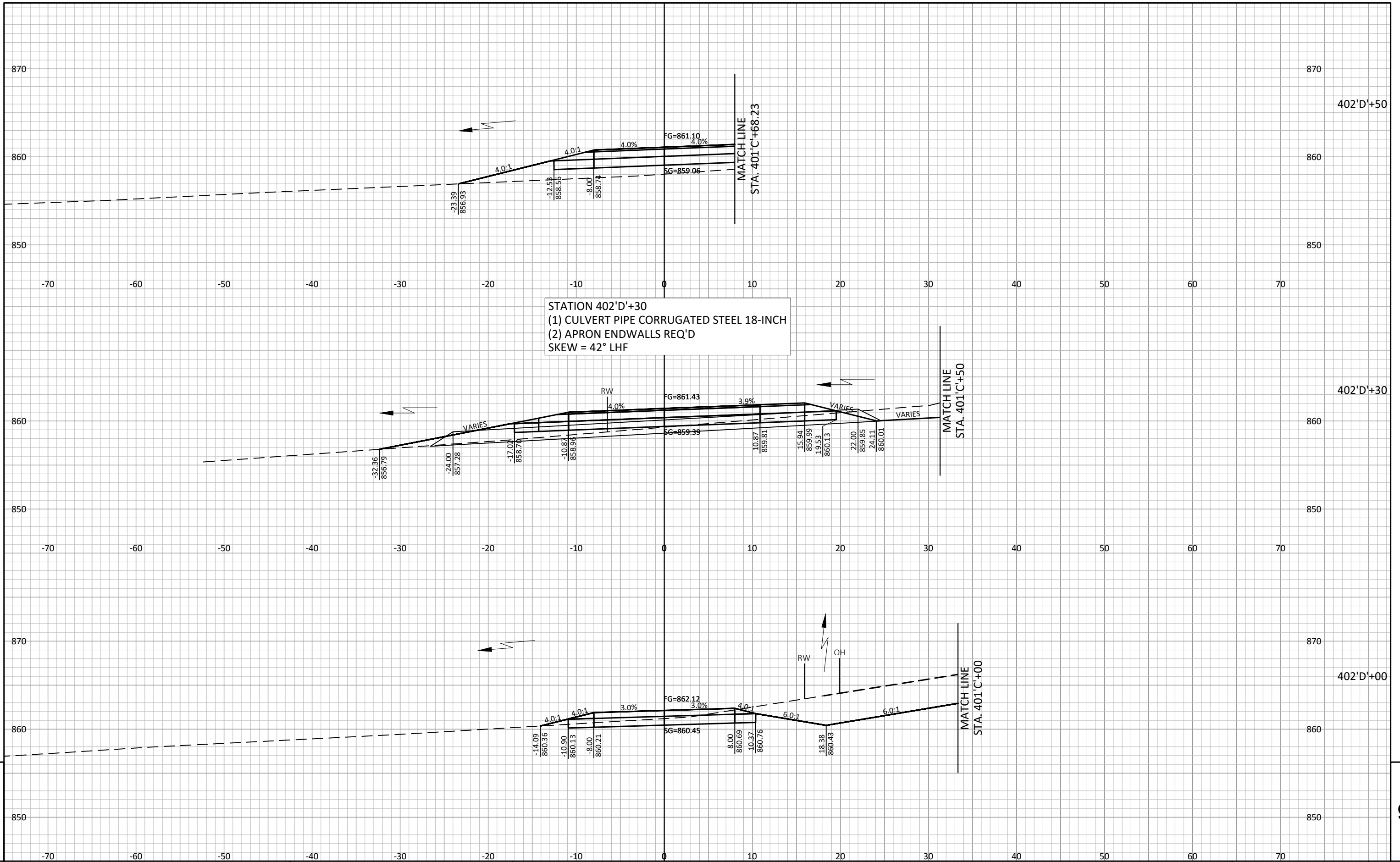
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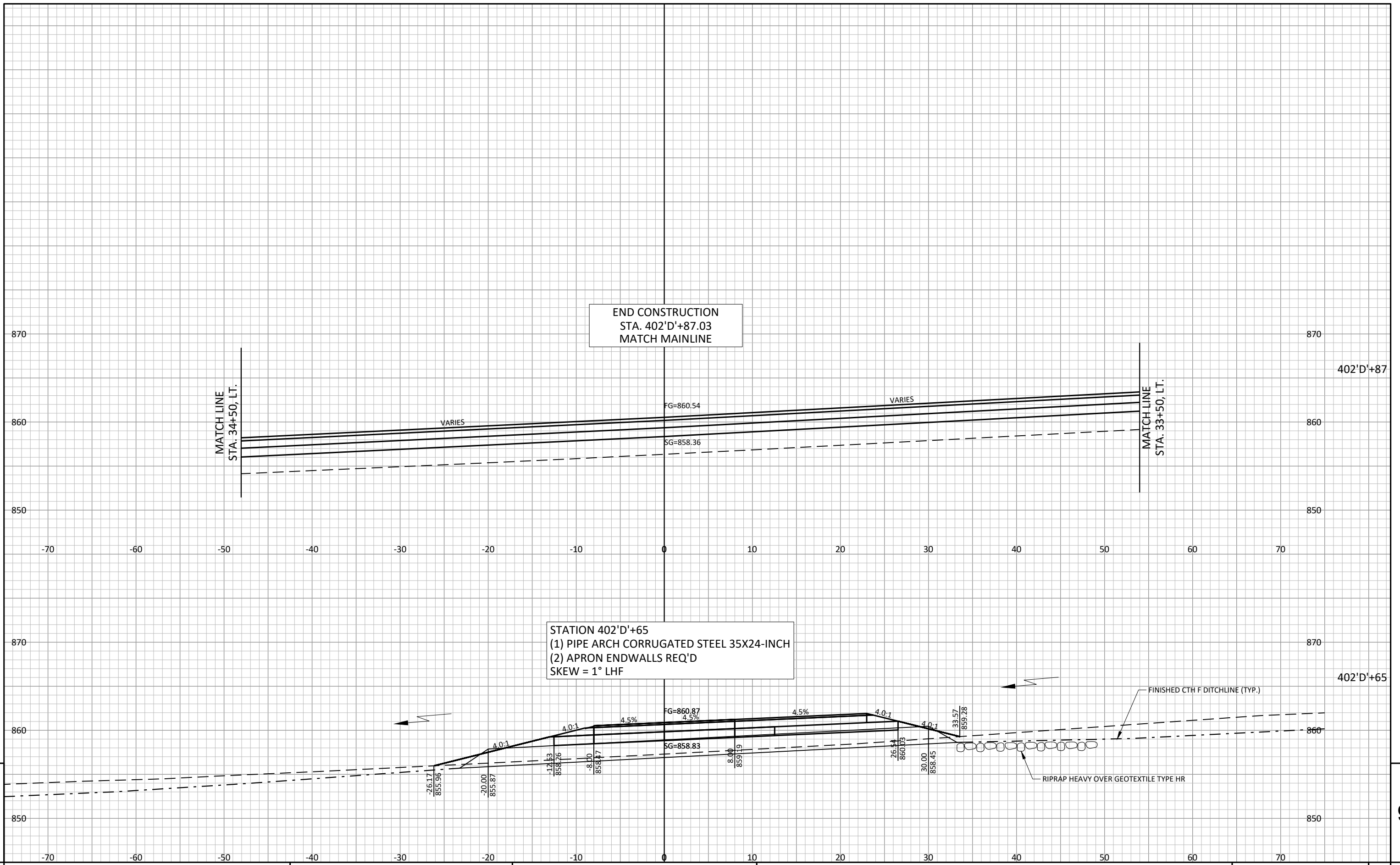
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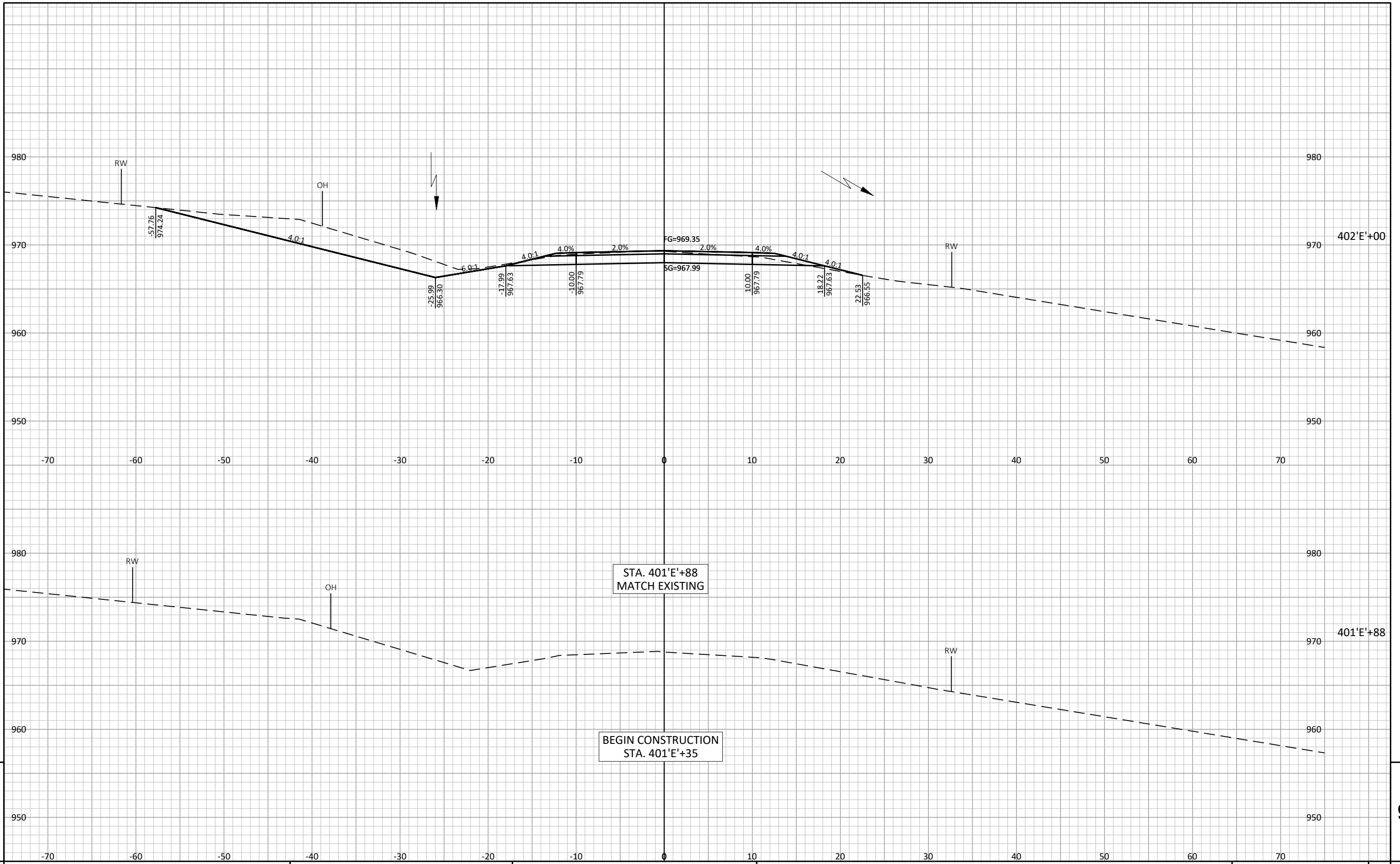
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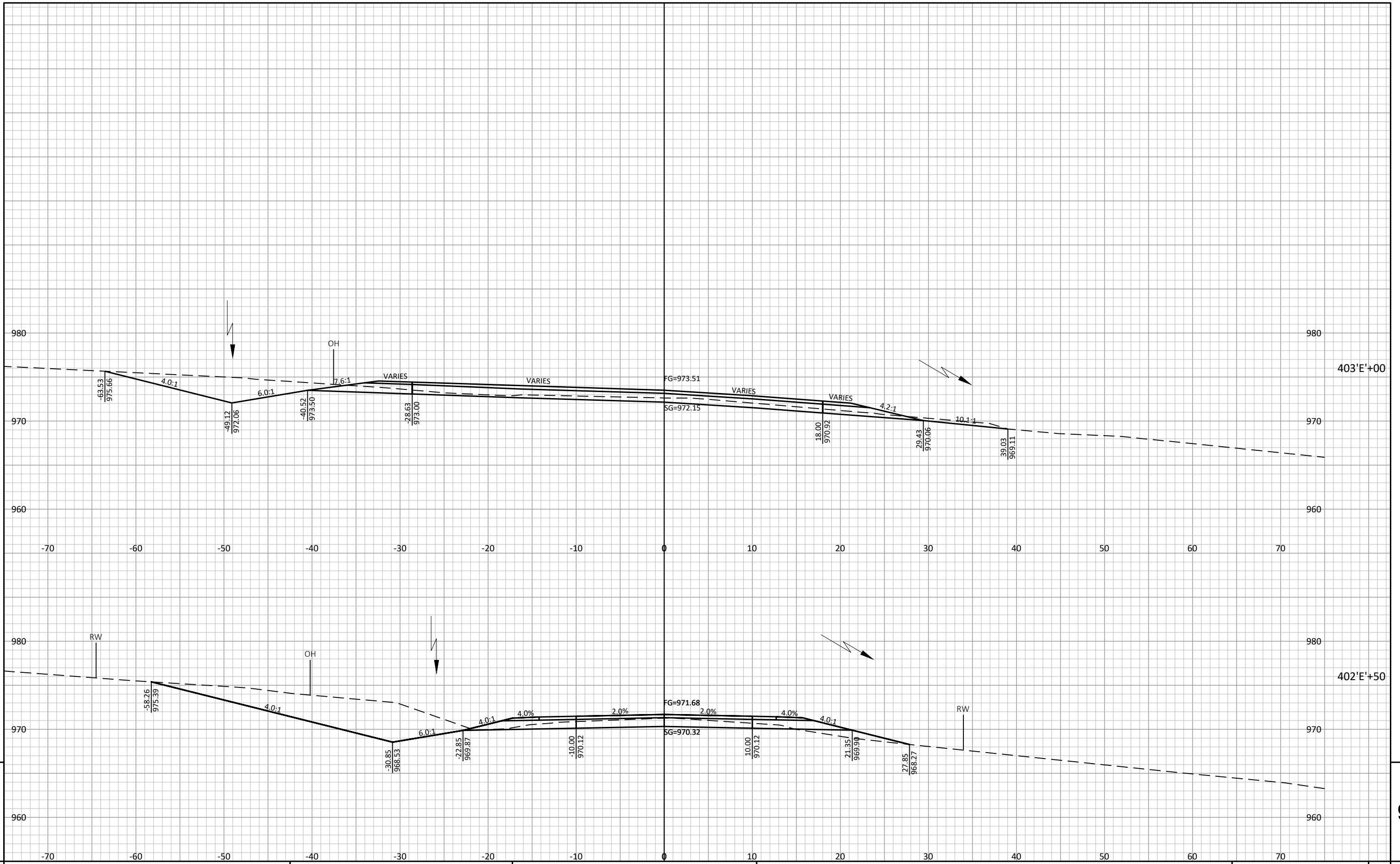
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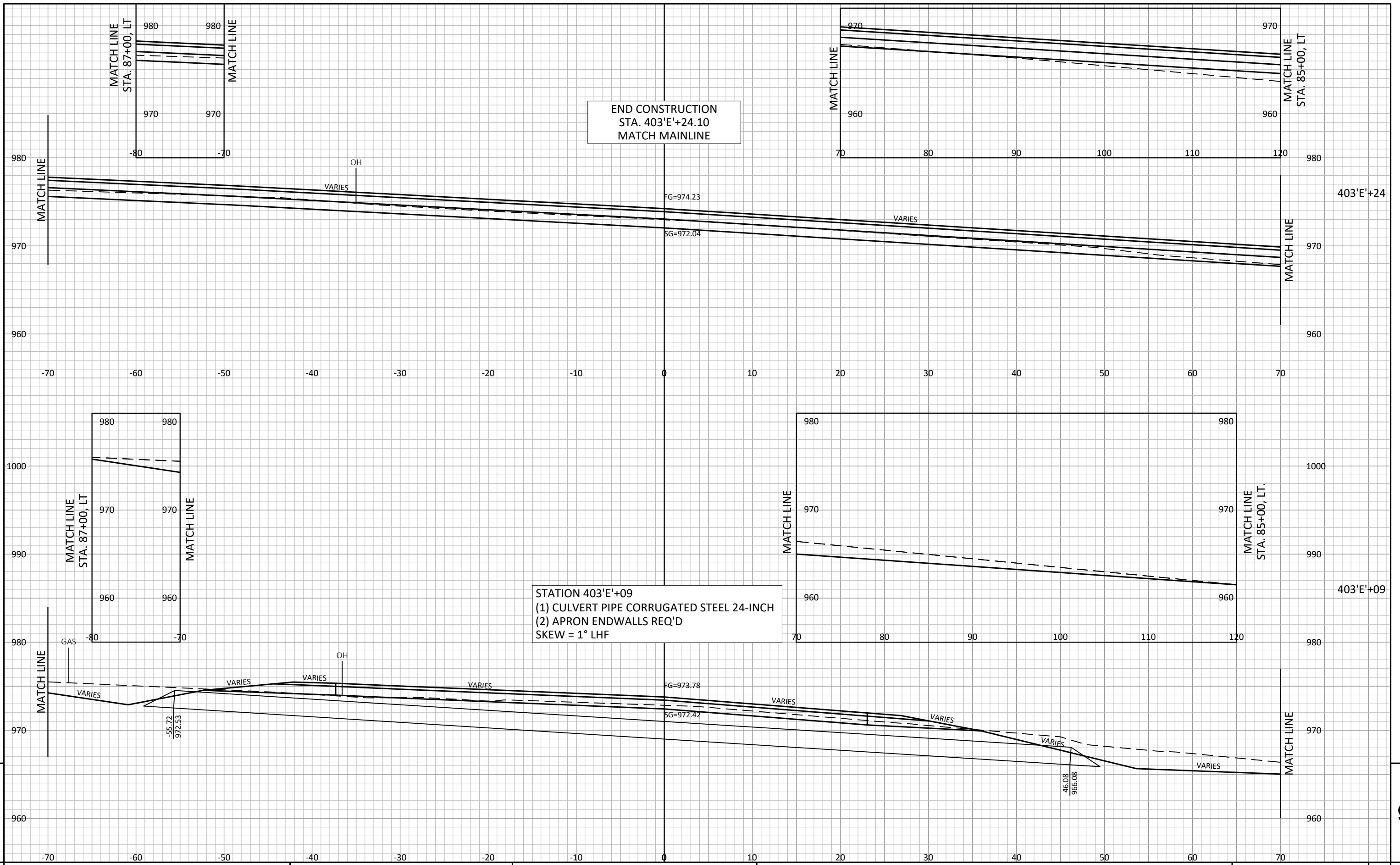
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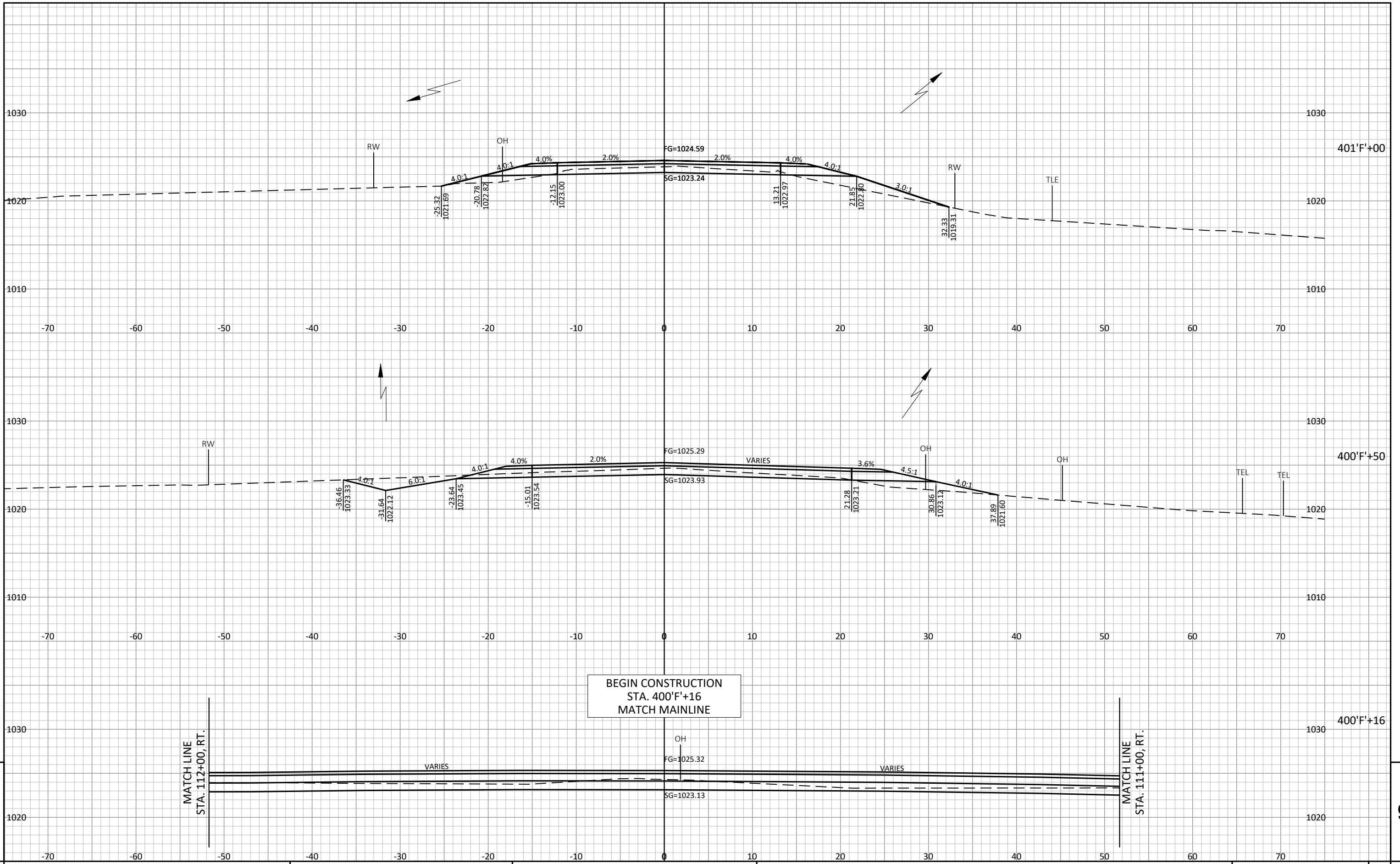
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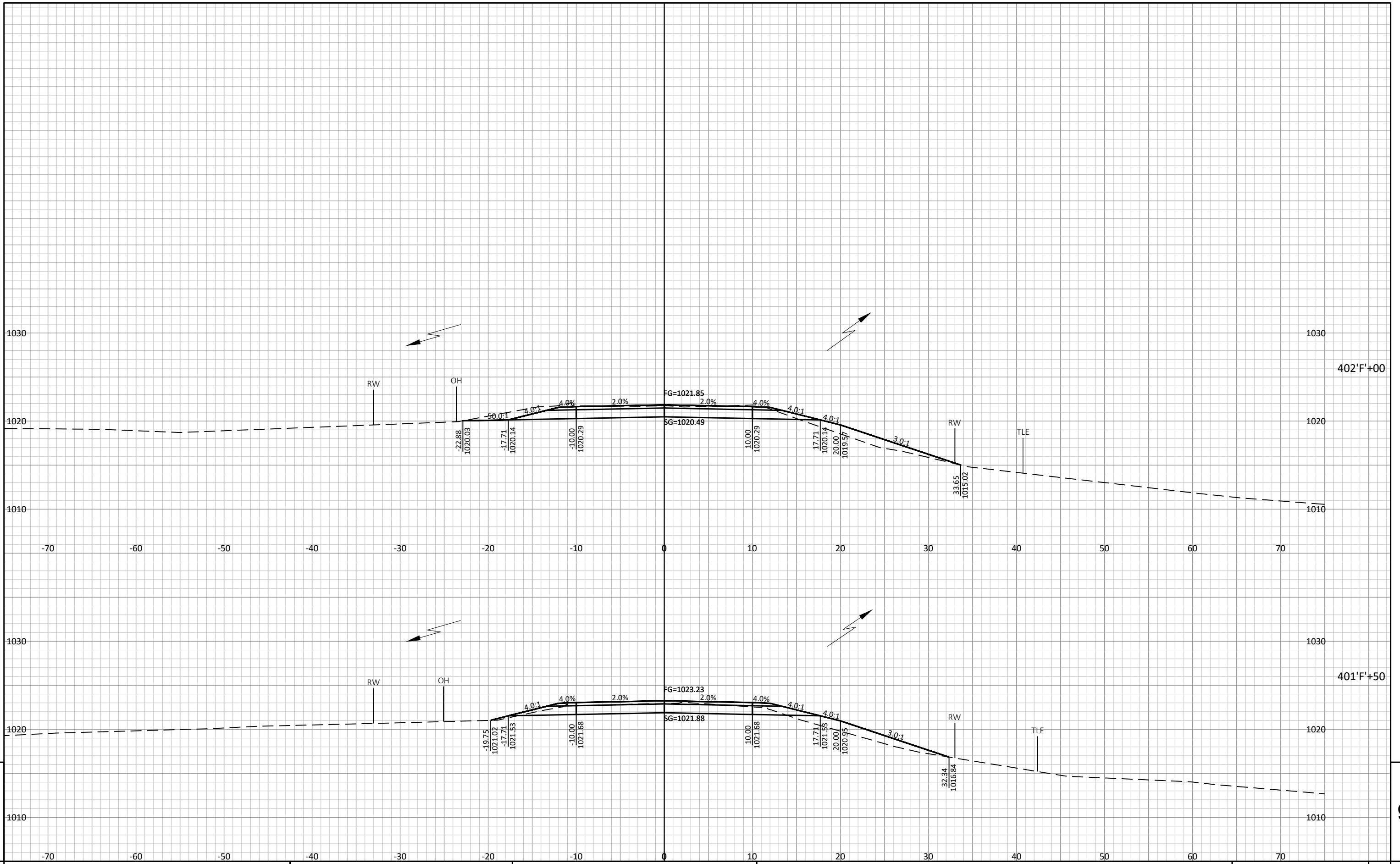
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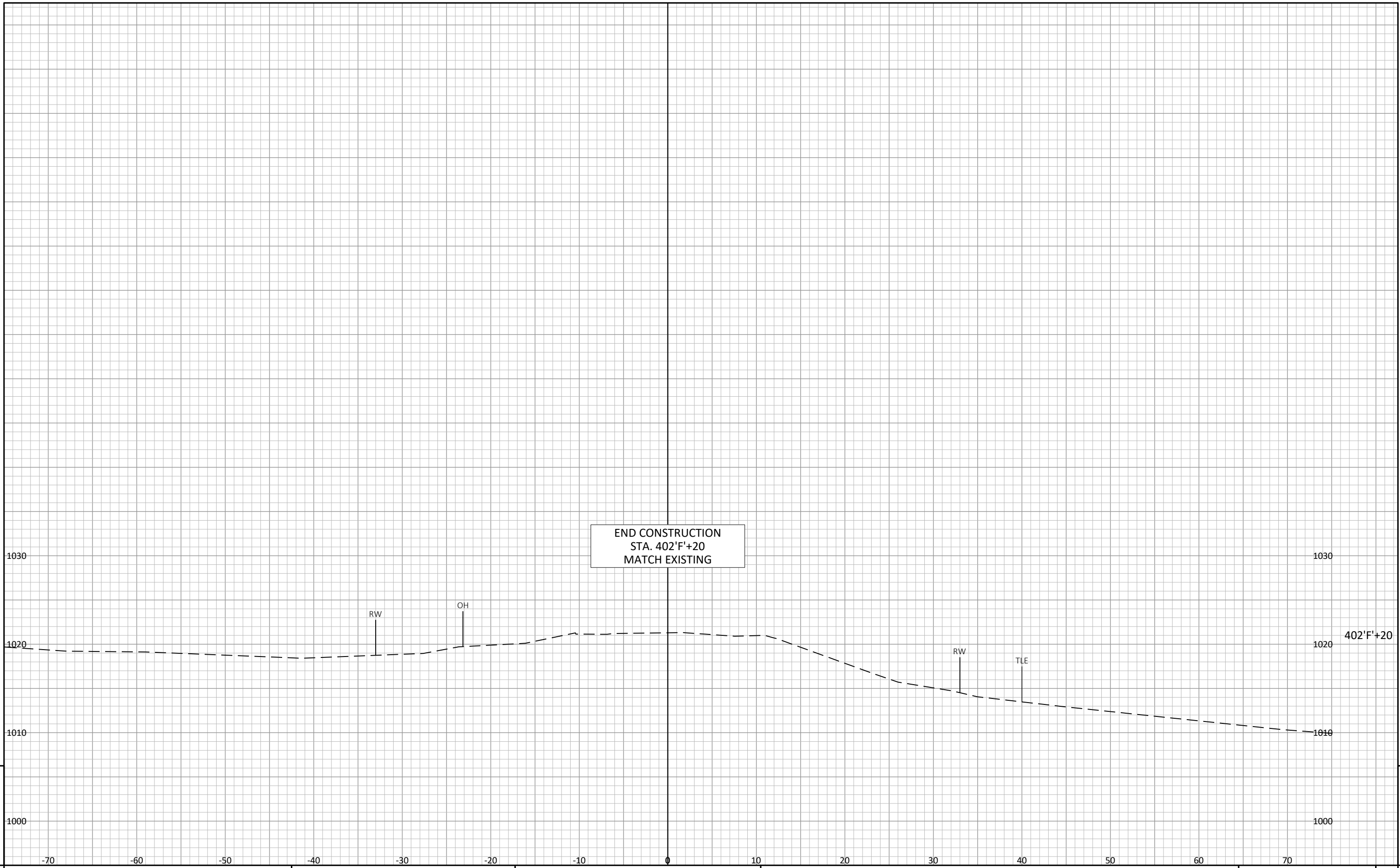
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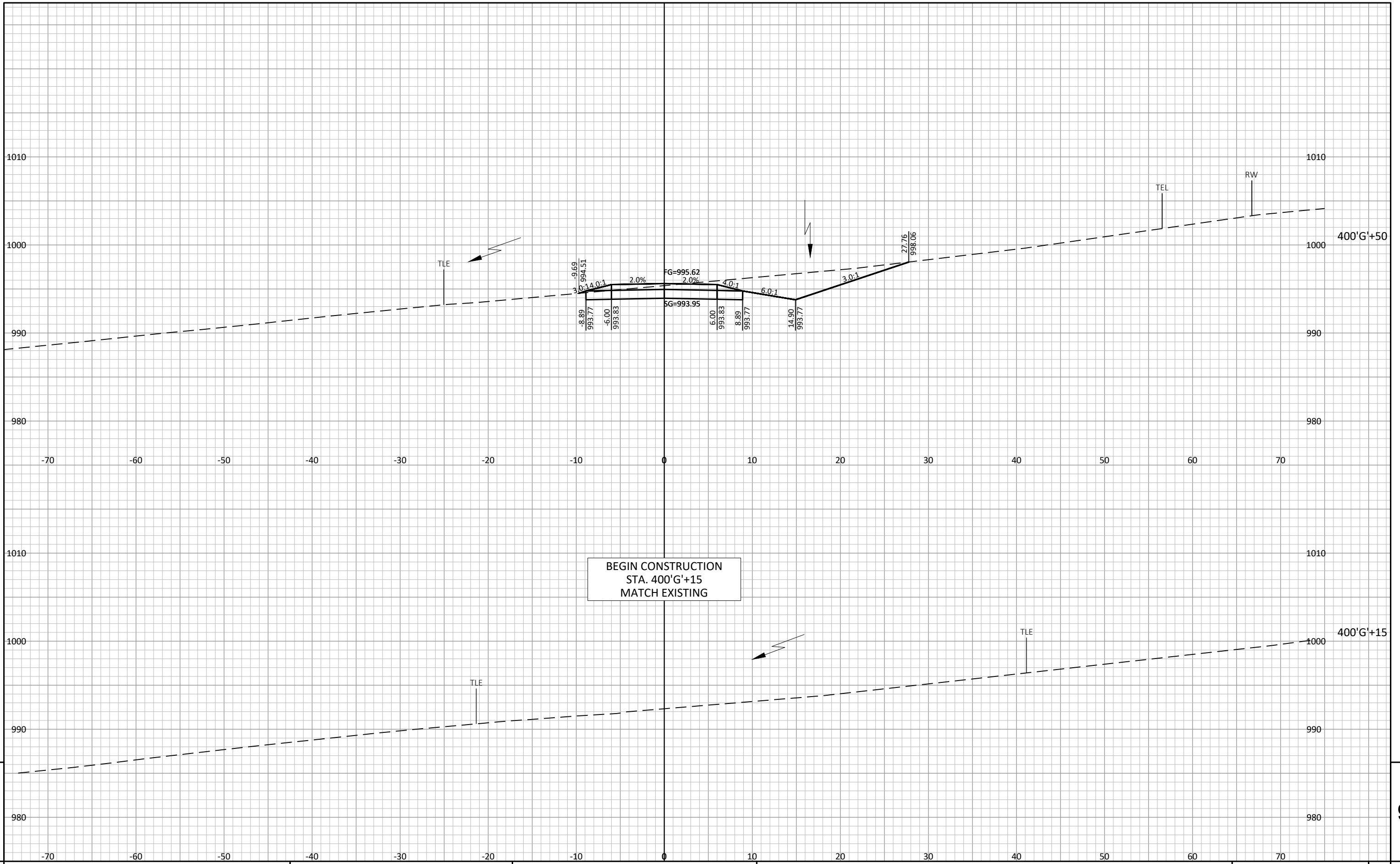
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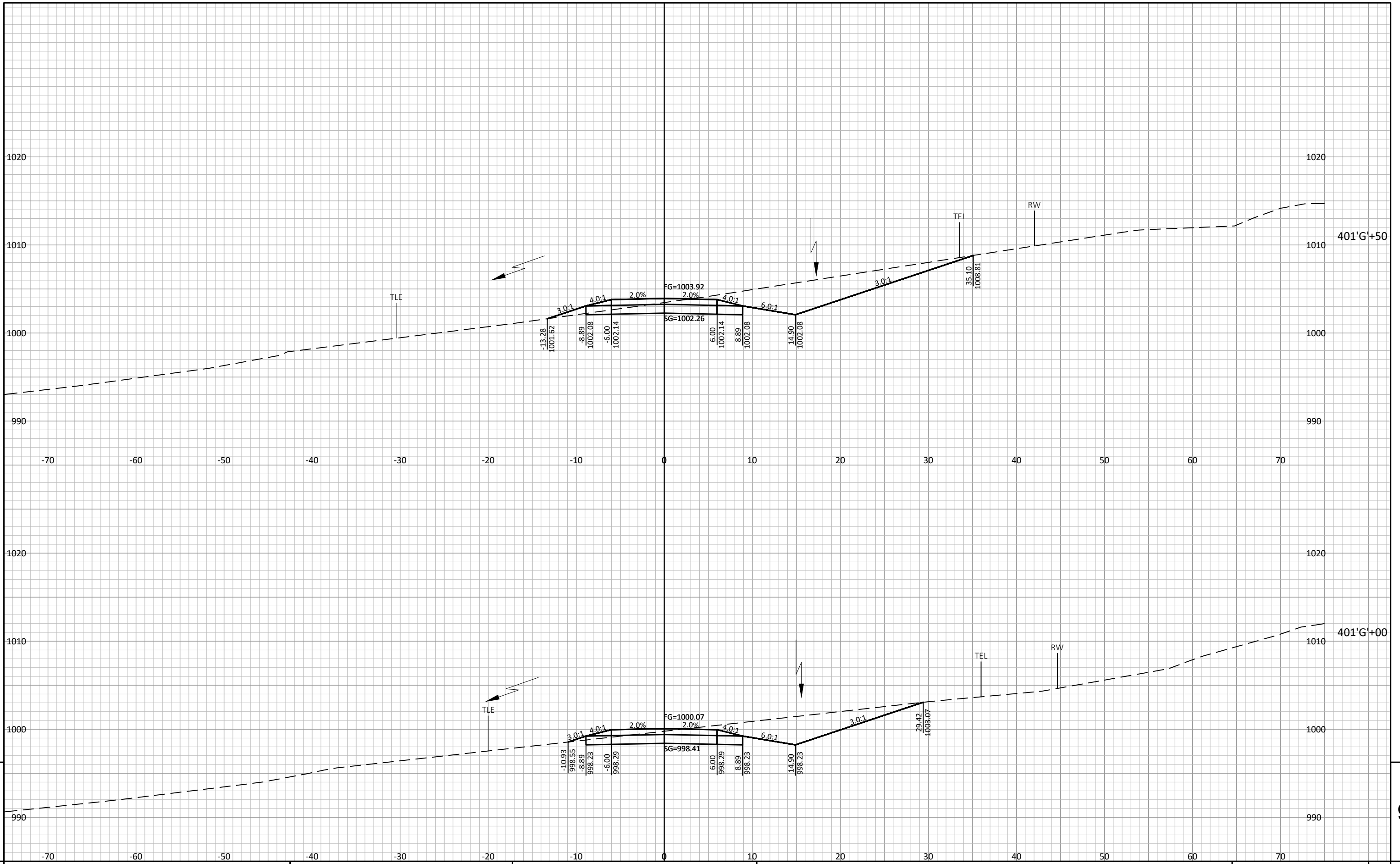
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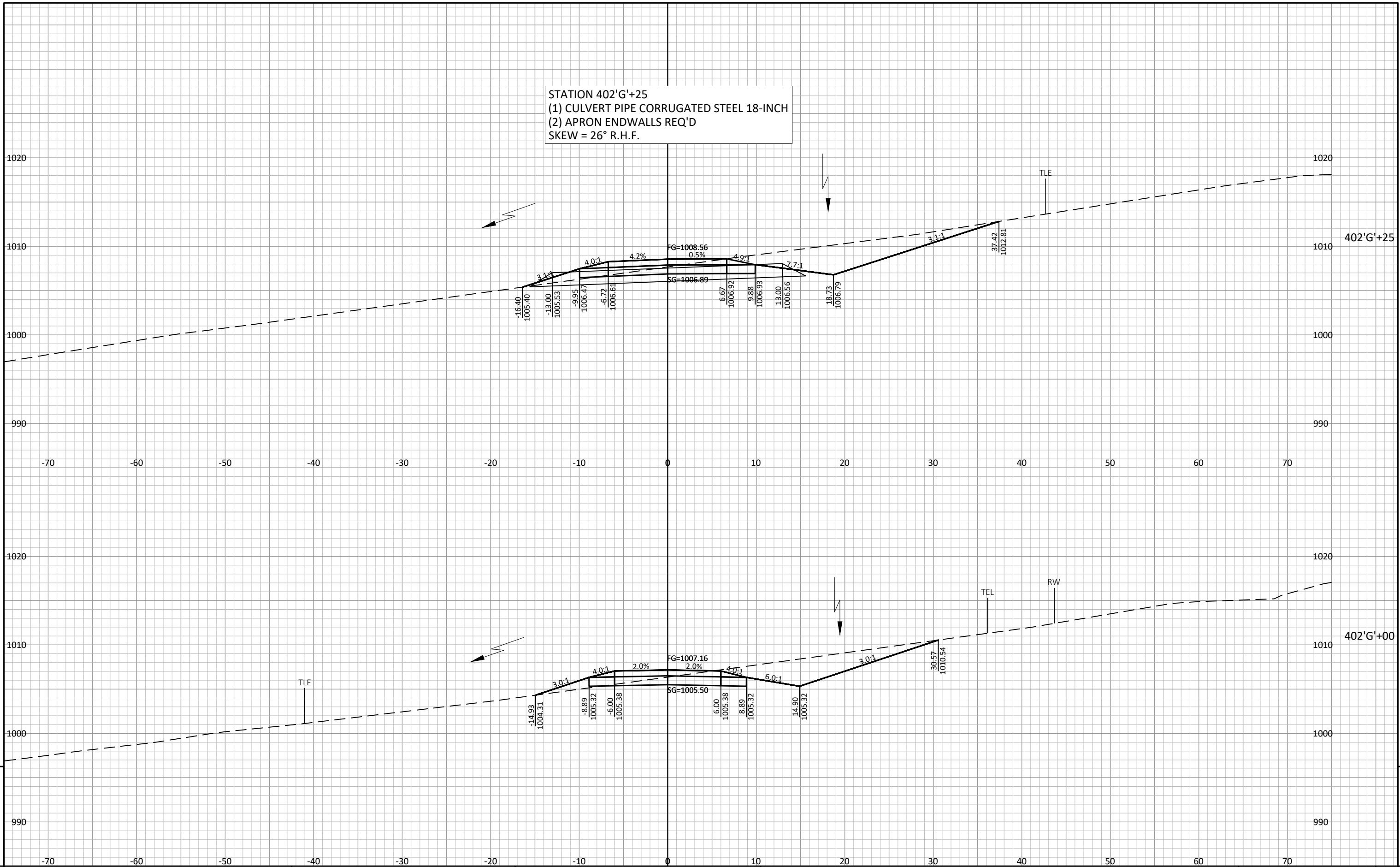
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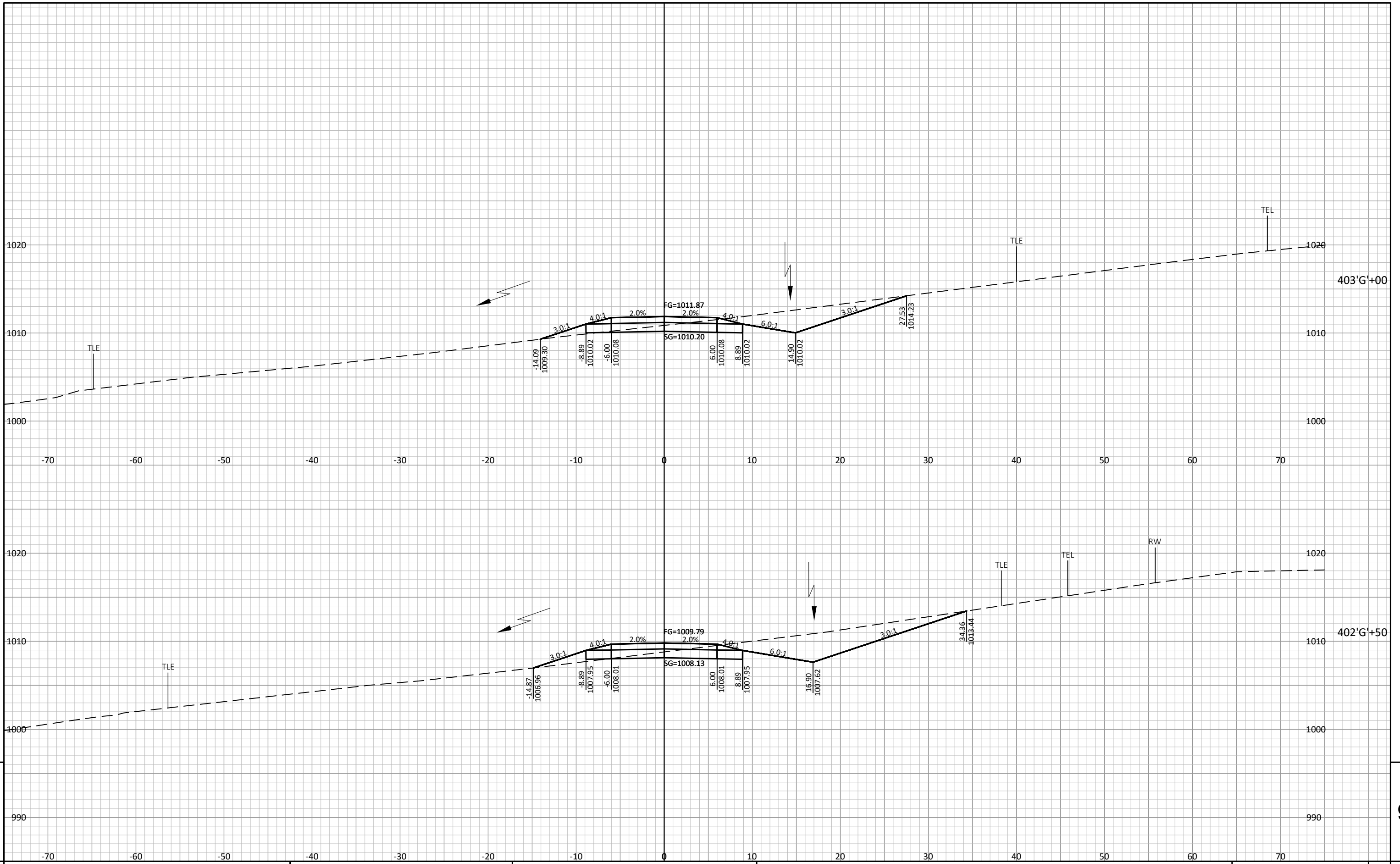
9

STATION 402'G'+25
 (1) CULVERT PIPE CORRUGATED STEEL 18-INCH
 (2) APRON ENDWALLS REQ'D
 SKEW = 26° R.H.F.



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PROJECT NO: 5034-00-72

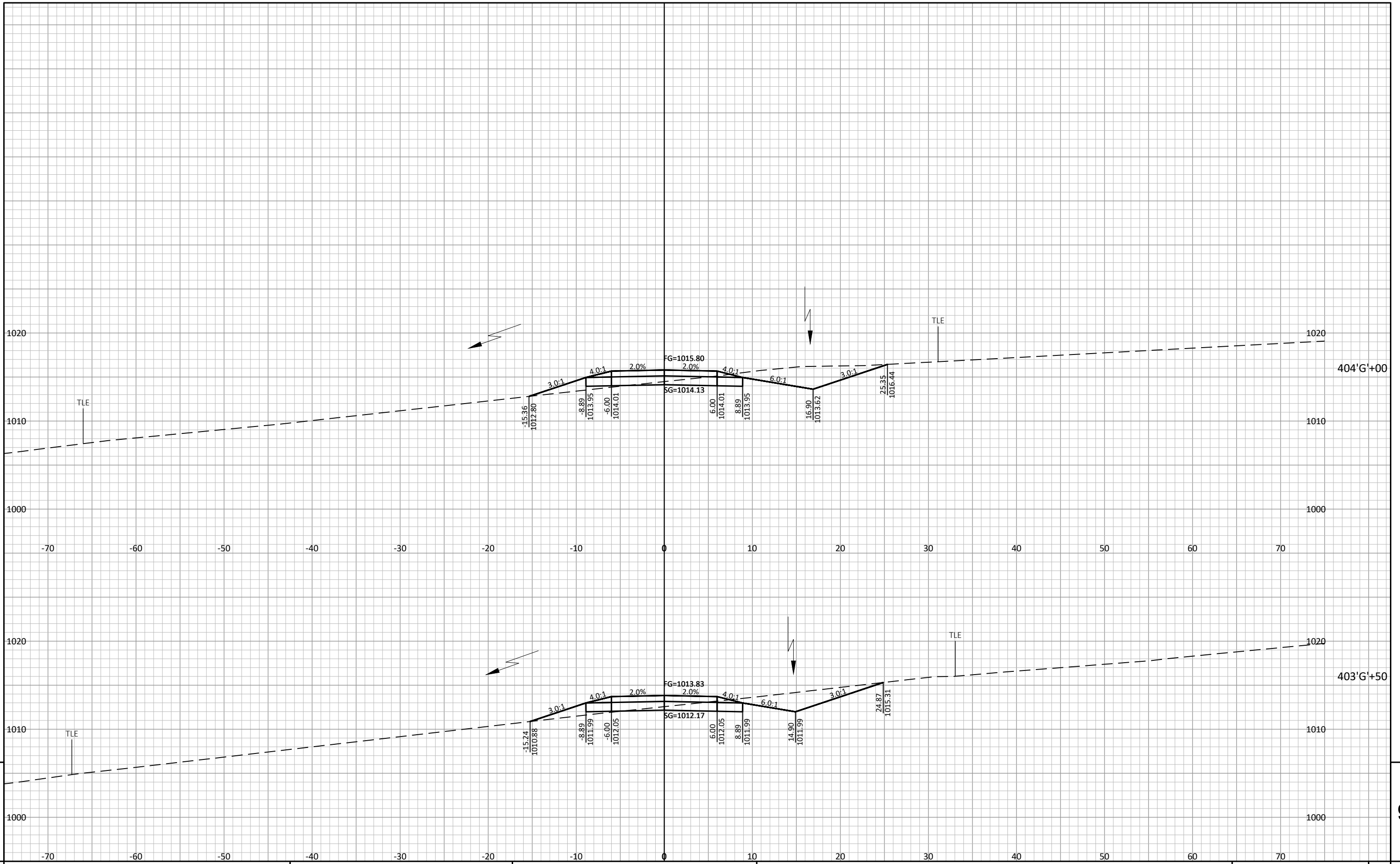
HWY: CTH F

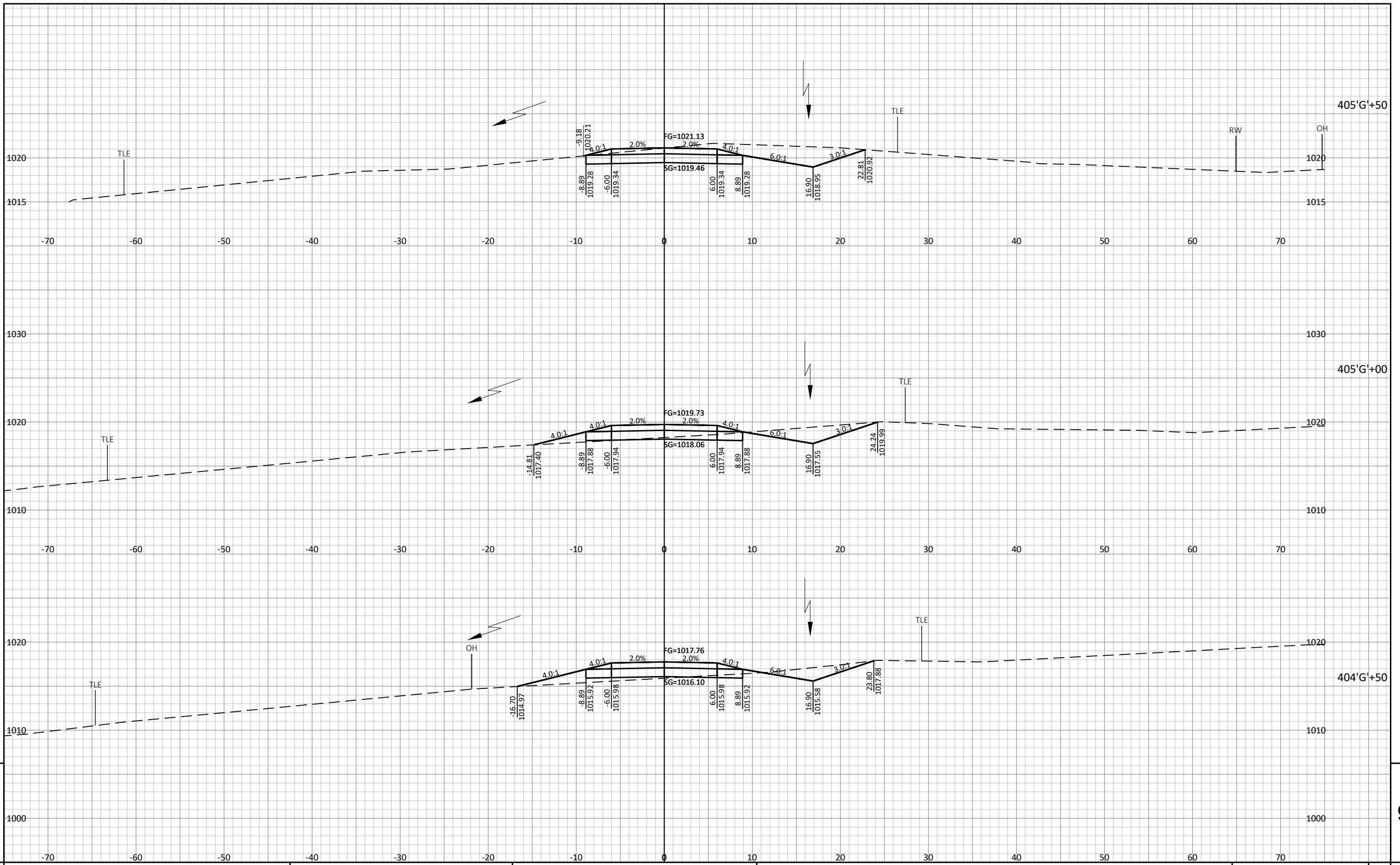
COUNTY: IOWA

CROSS SECTIONS: 'G'-LINE (DRIVEWAY)

SHEET

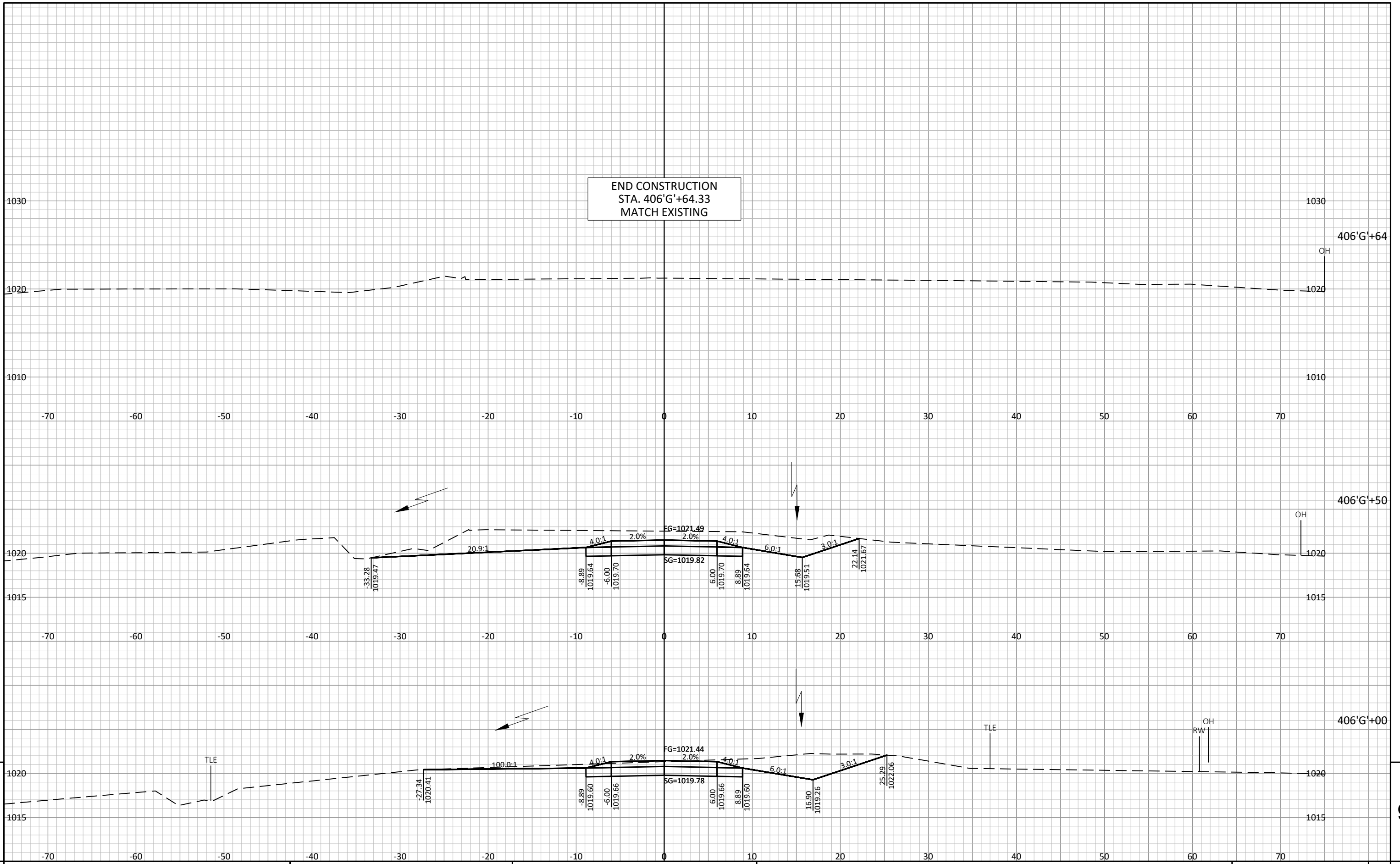
E





9

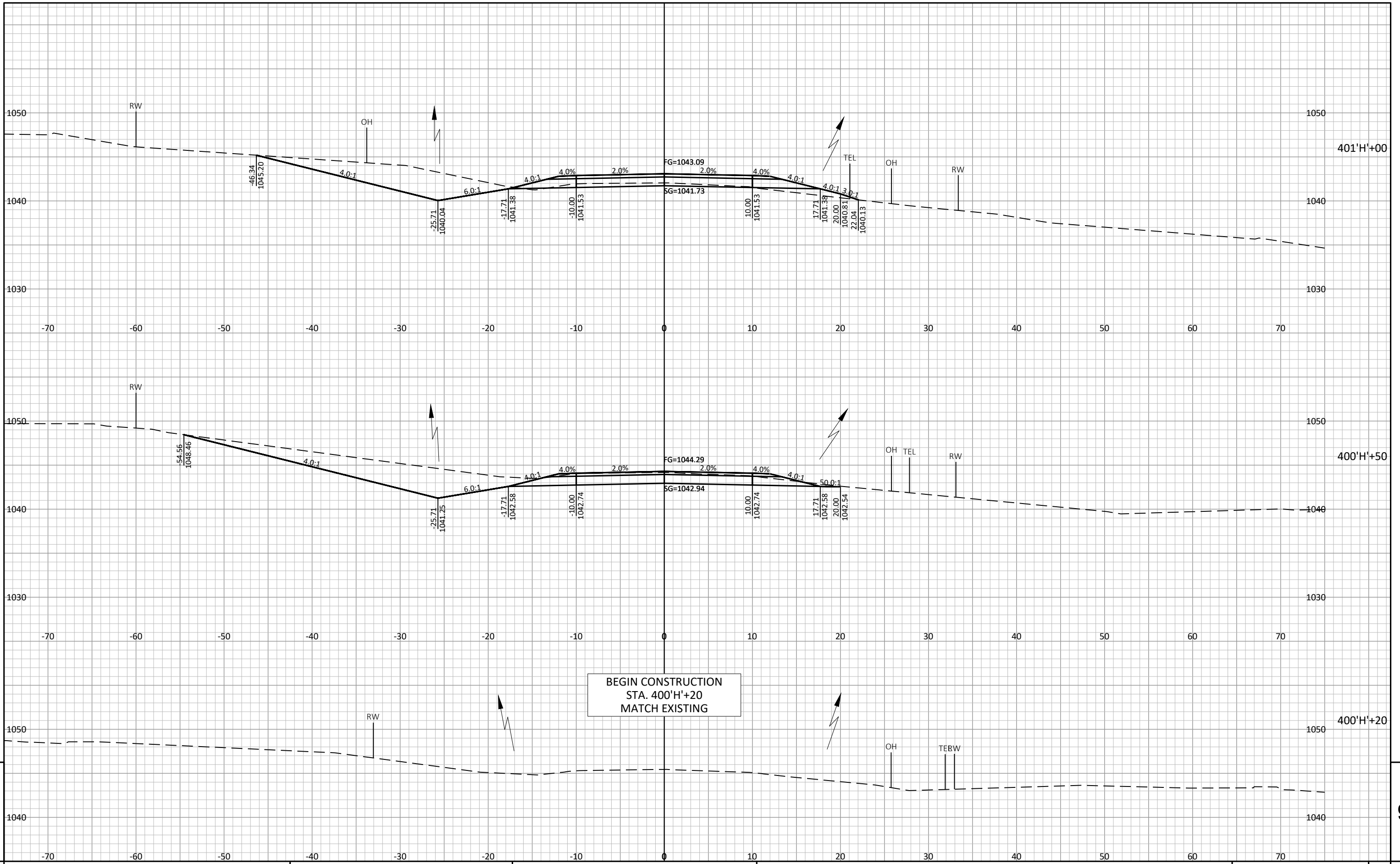
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END CONSTRUCTION
 STA. 406'G'+64.33
 MATCH EXISTING

9

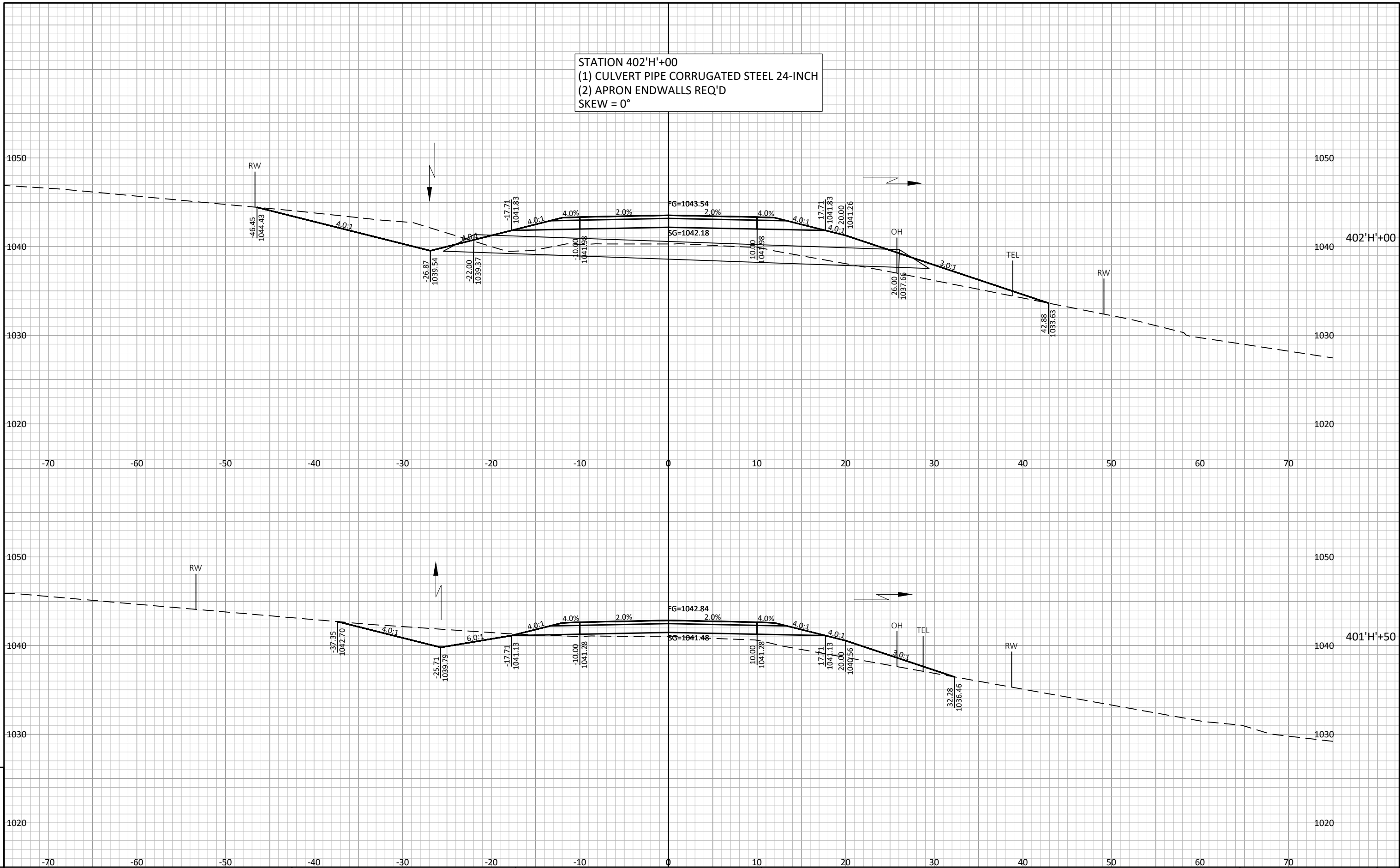
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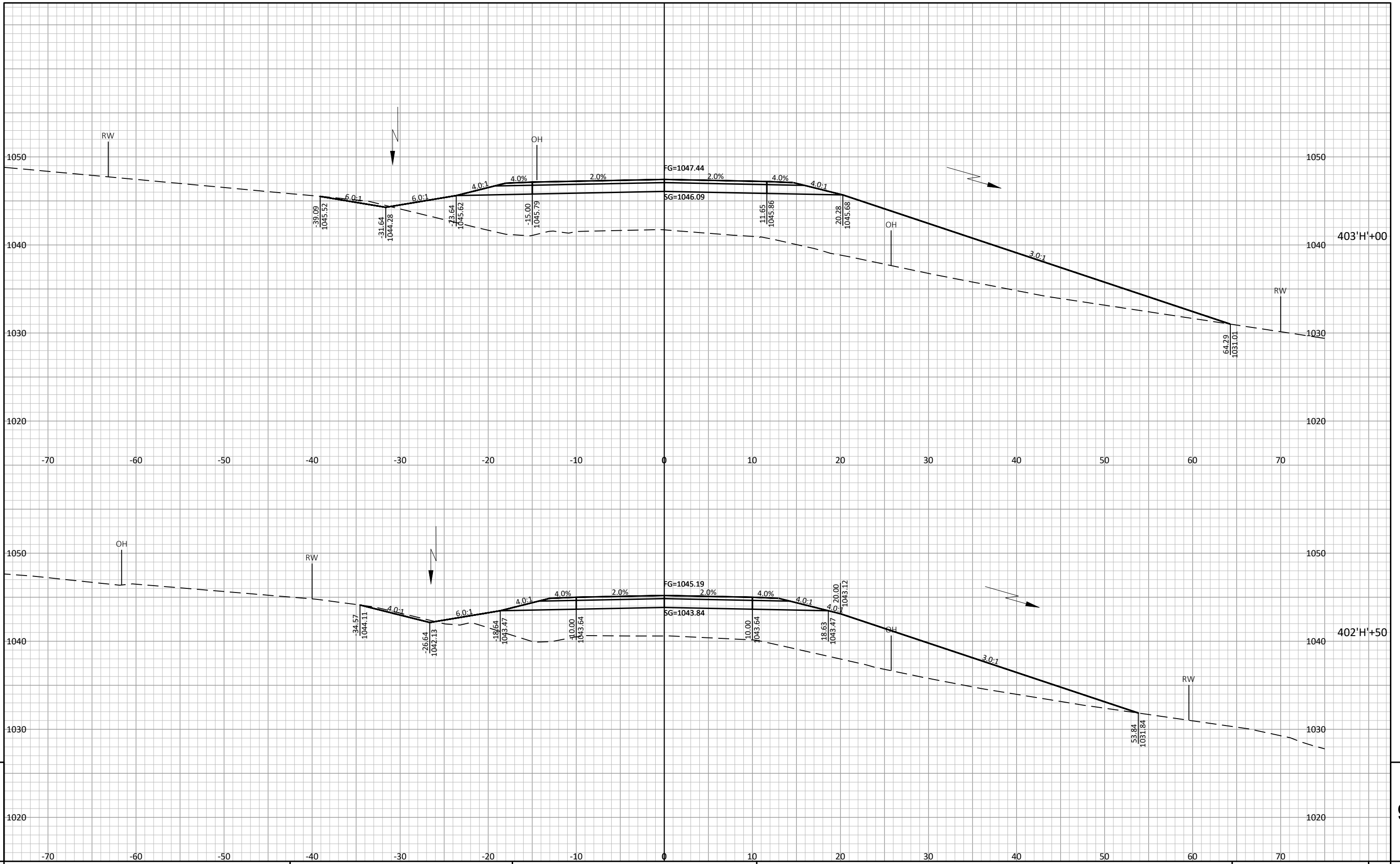
9

STATION 402'H'+00
 (1) CULVERT PIPE CORRUGATED STEEL 24-INCH
 (2) APRON ENDWALLS REQ'D
 SKEW = 0°



9

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PROJECT NO: 5034-00-72

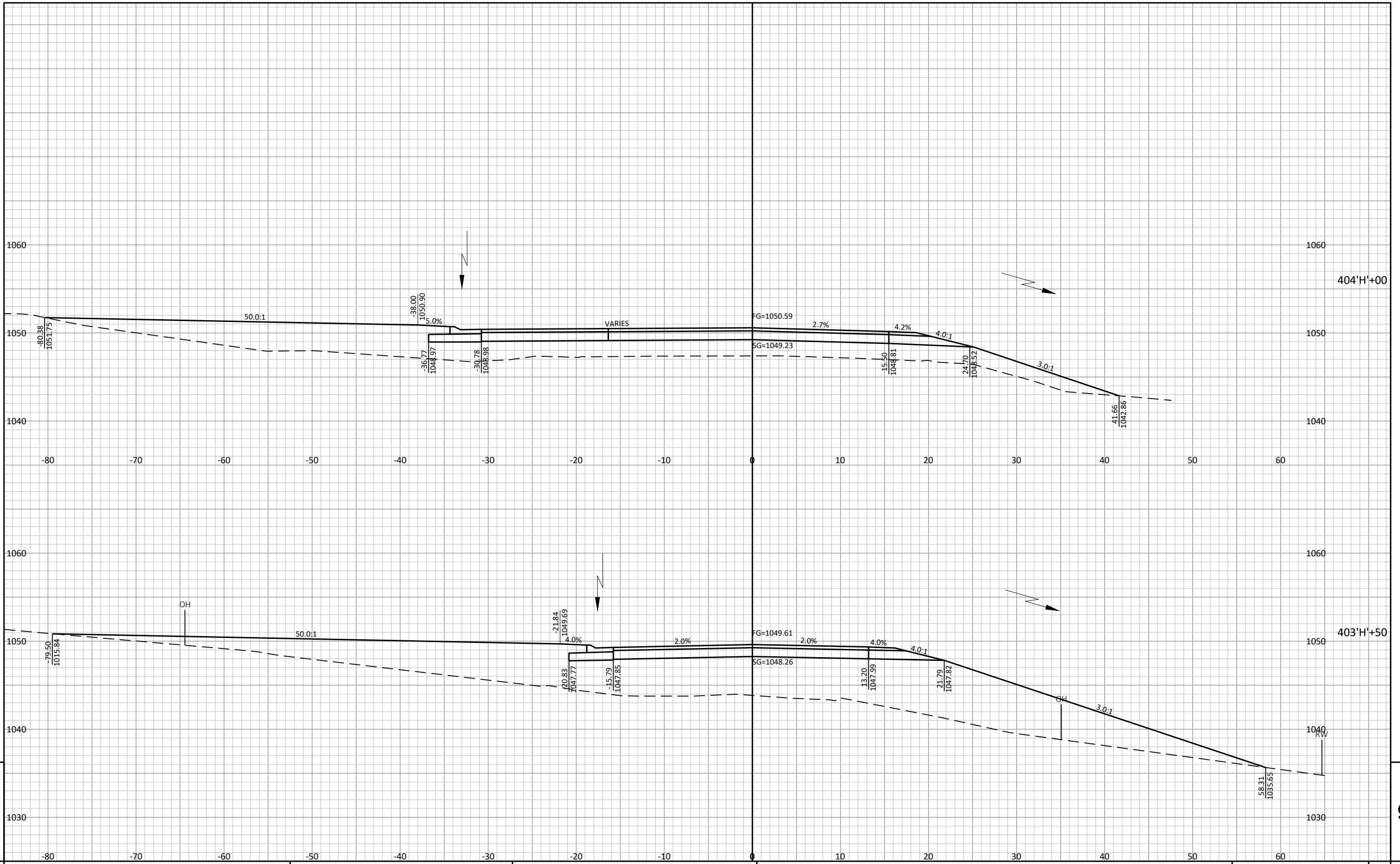
HWY: CTH F

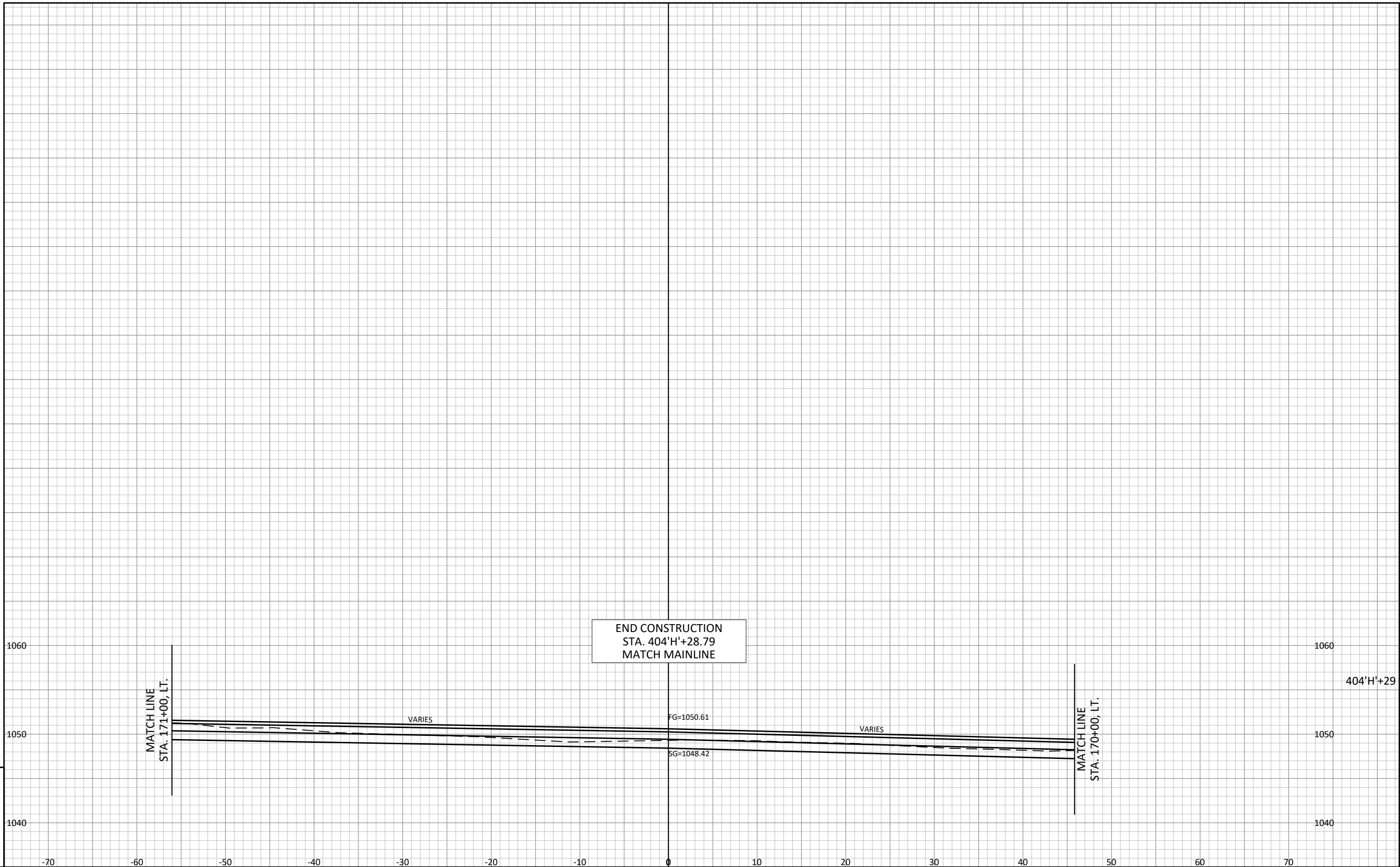
COUNTY: IOWA

CROSS SECTIONS: 'H'-LINE (MCKENNA ROAD)

SHEET

E





9

9

PROJECT NO: 5034-00-72

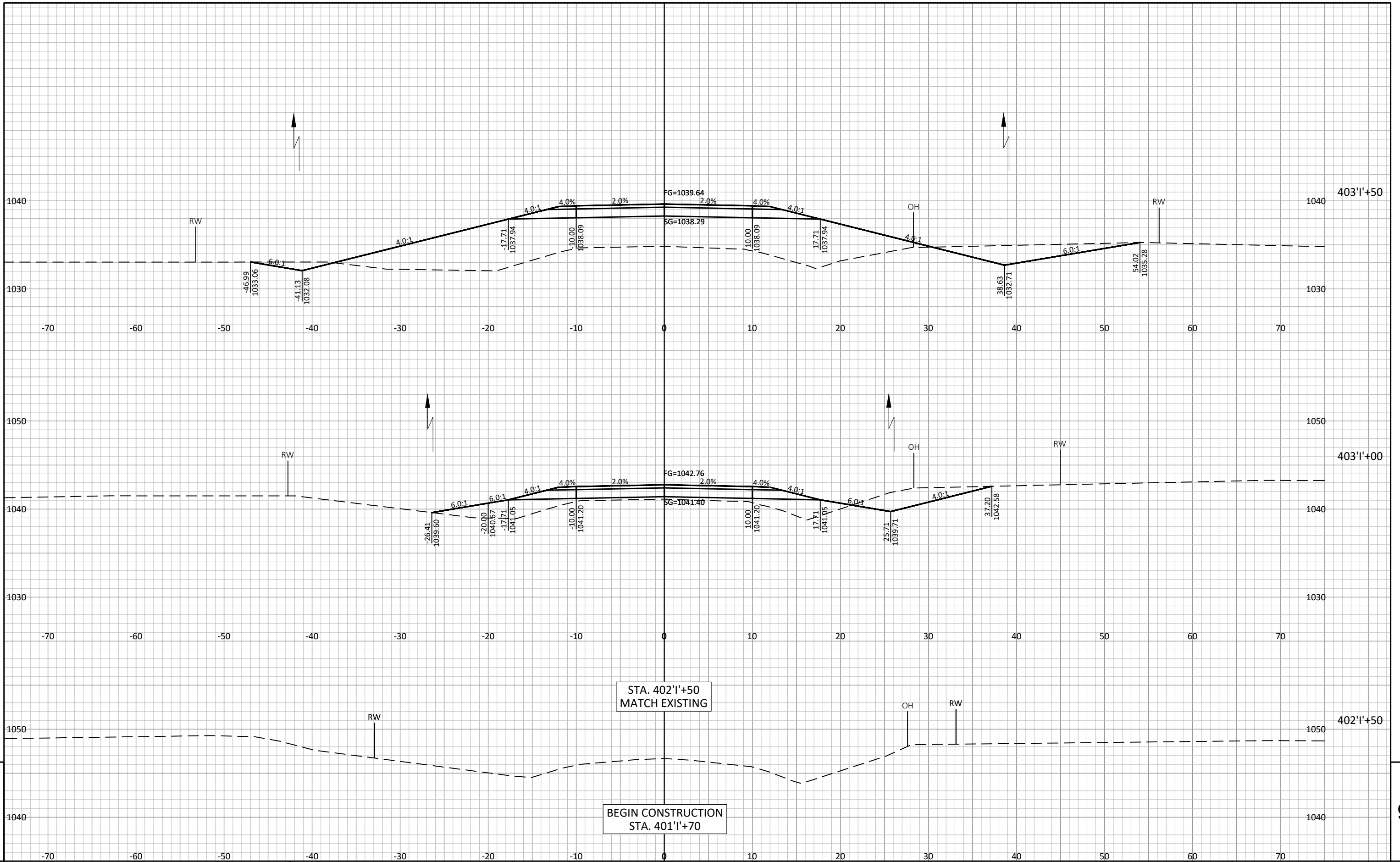
HWY: CTH F

COUNTY: IOWA

CROSS SECTIONS: 'H'-LINE (MCKENNA ROAD)

SHEET

E

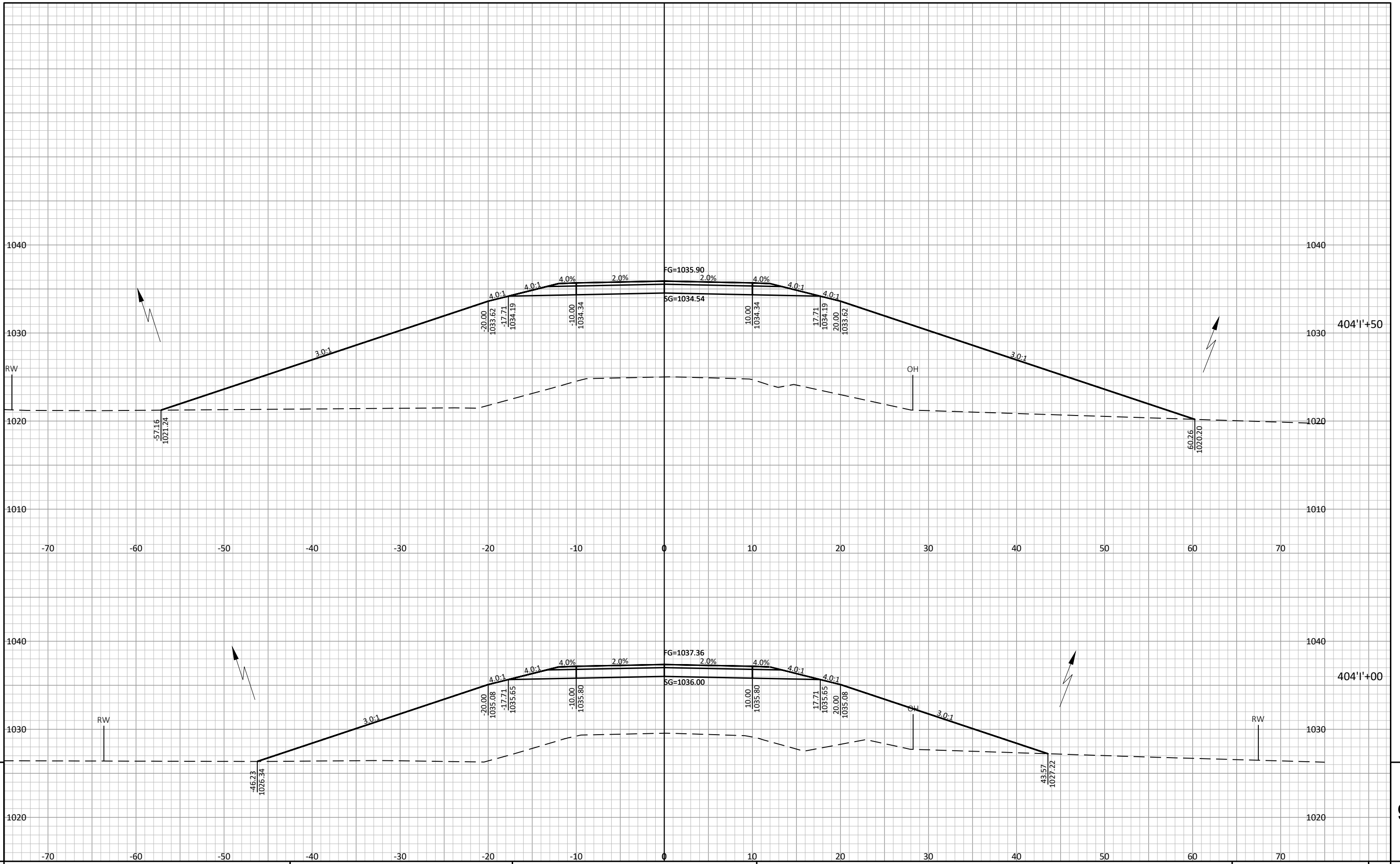


STA. 402'+50
MATCH EXISTING

BEGIN CONSTRUCTION
STA. 401'+70

9

9



PROJECT NO: 5034-00-72

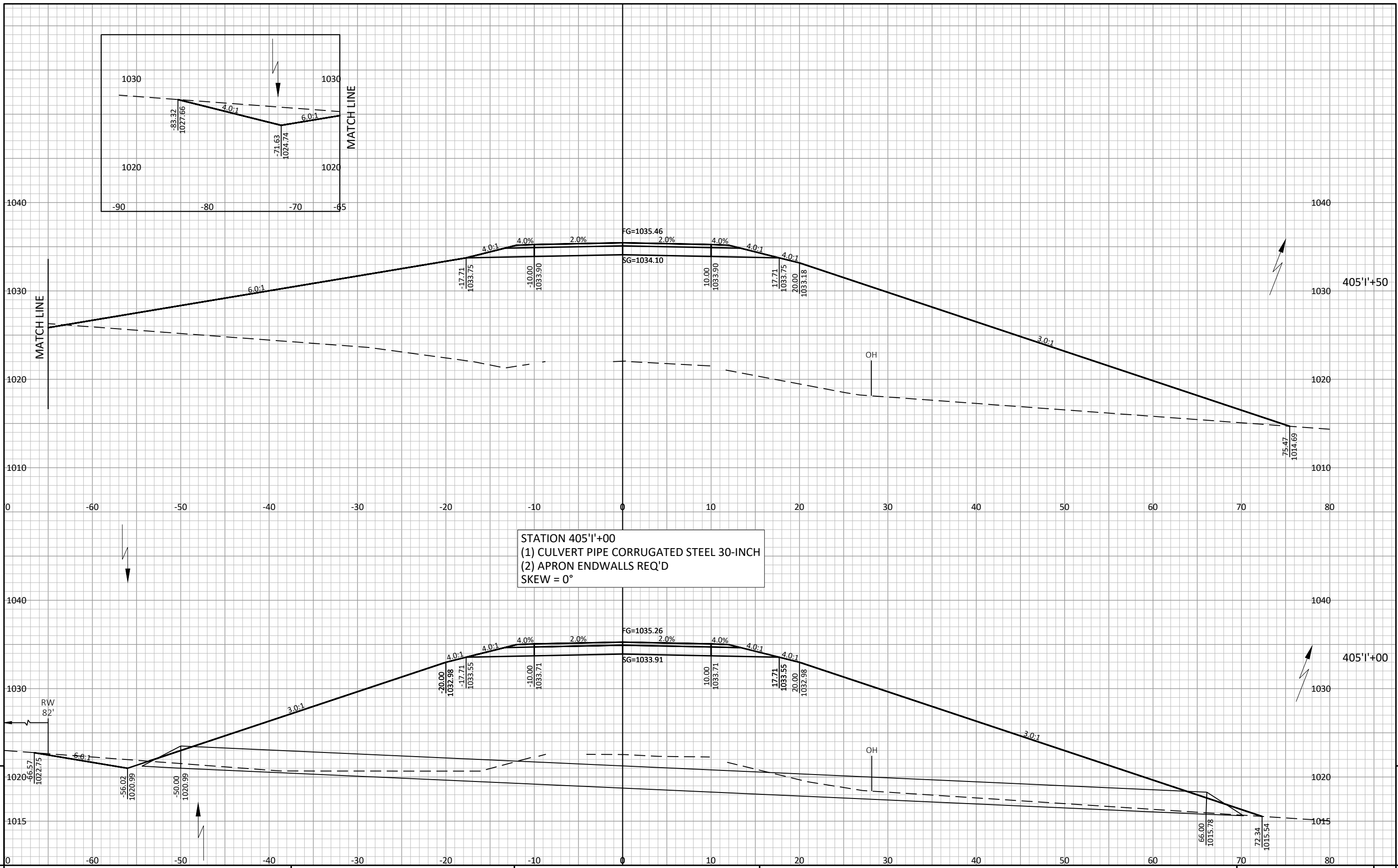
HWY: CTH F

COUNTY: IOWA

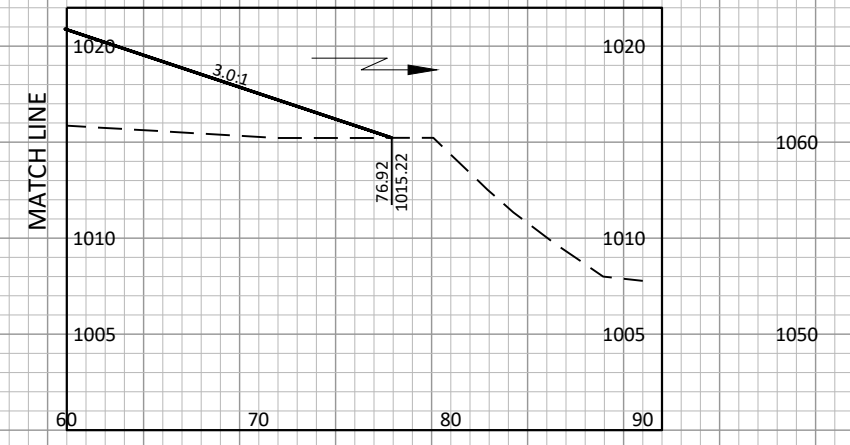
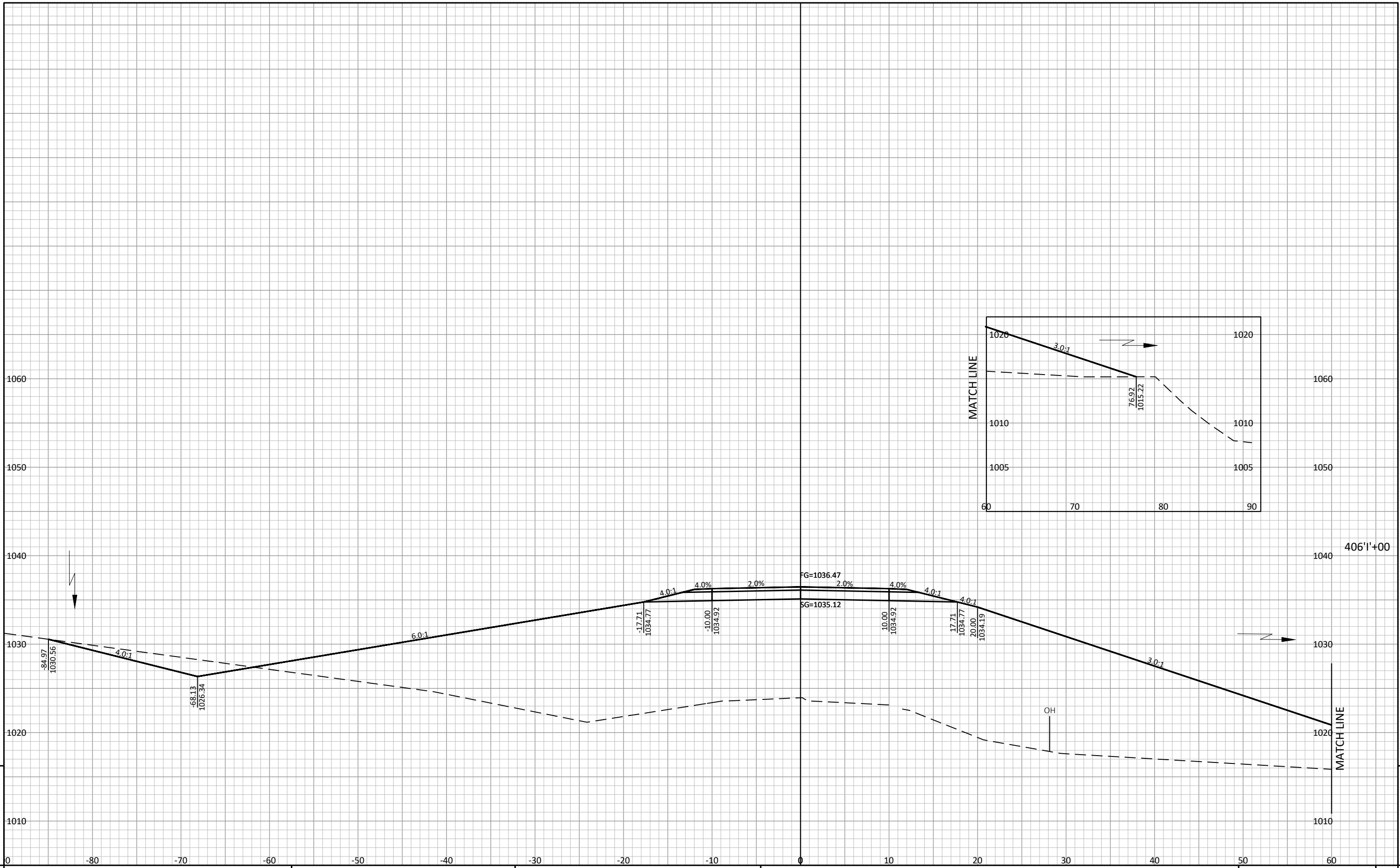
CROSS SECTIONS: 'I'-LINE (EAST PECATONICA ROAD)

SHEET

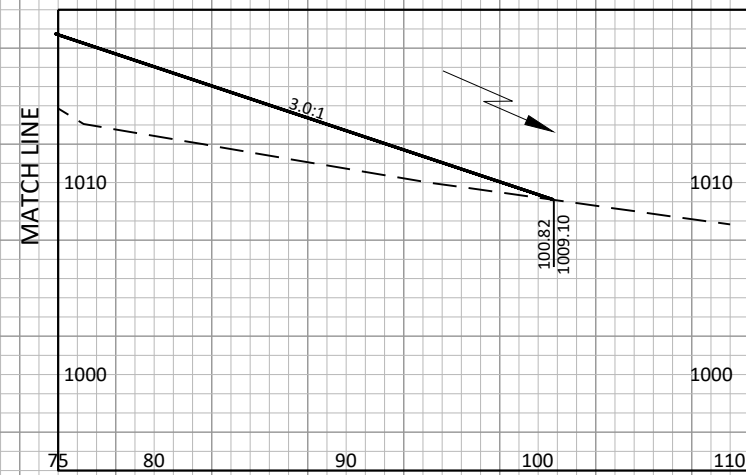
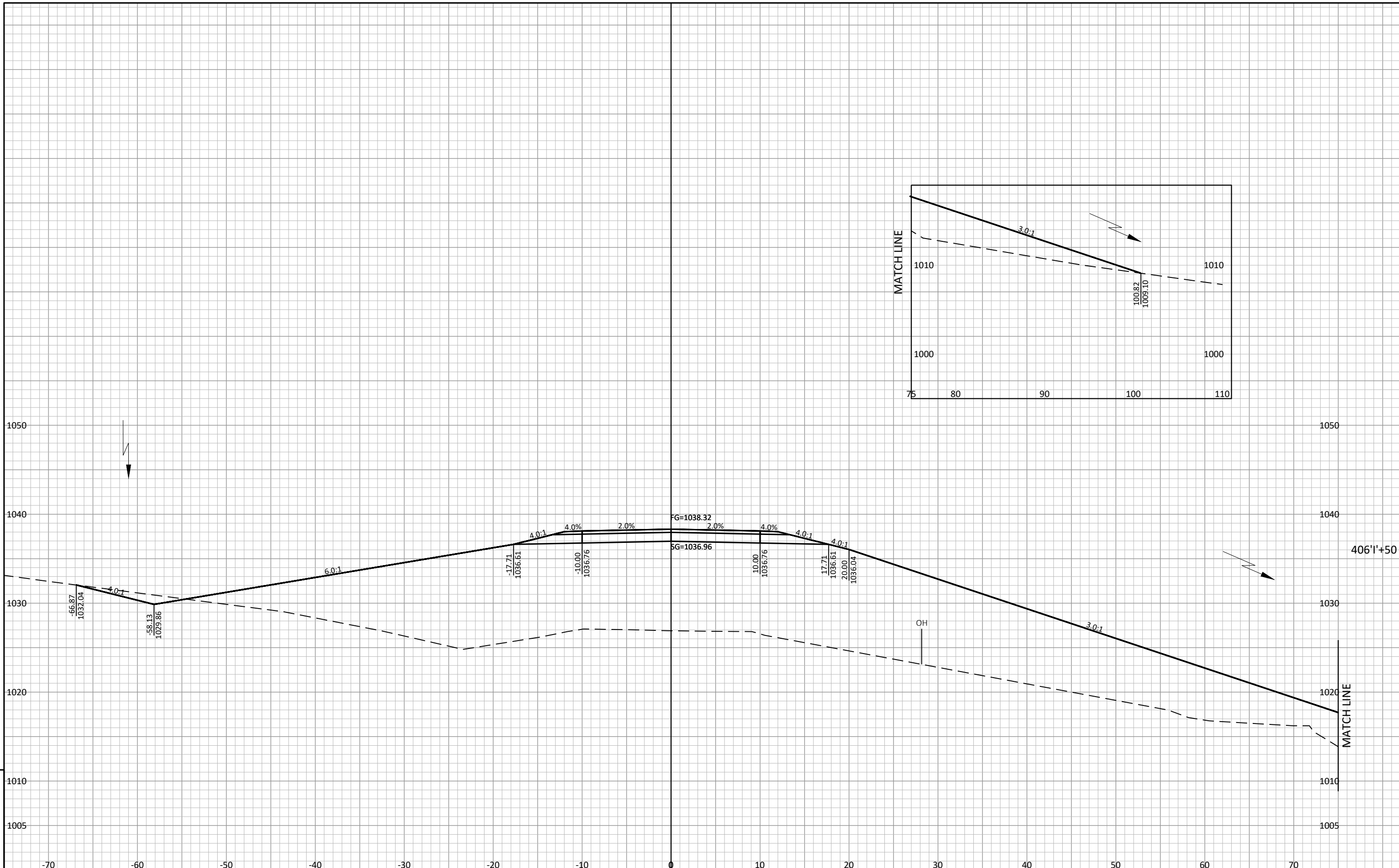
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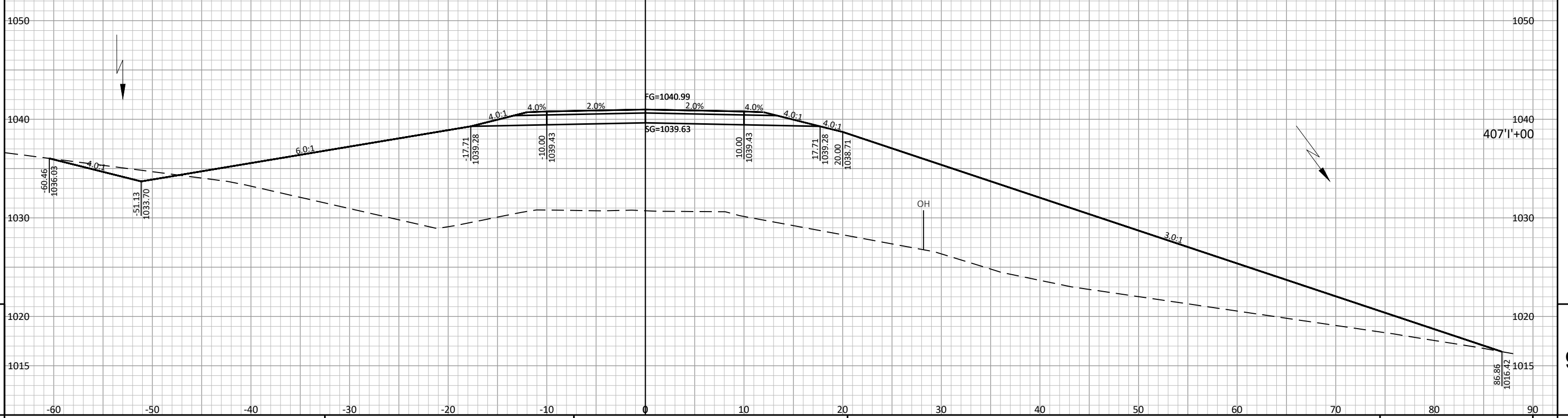
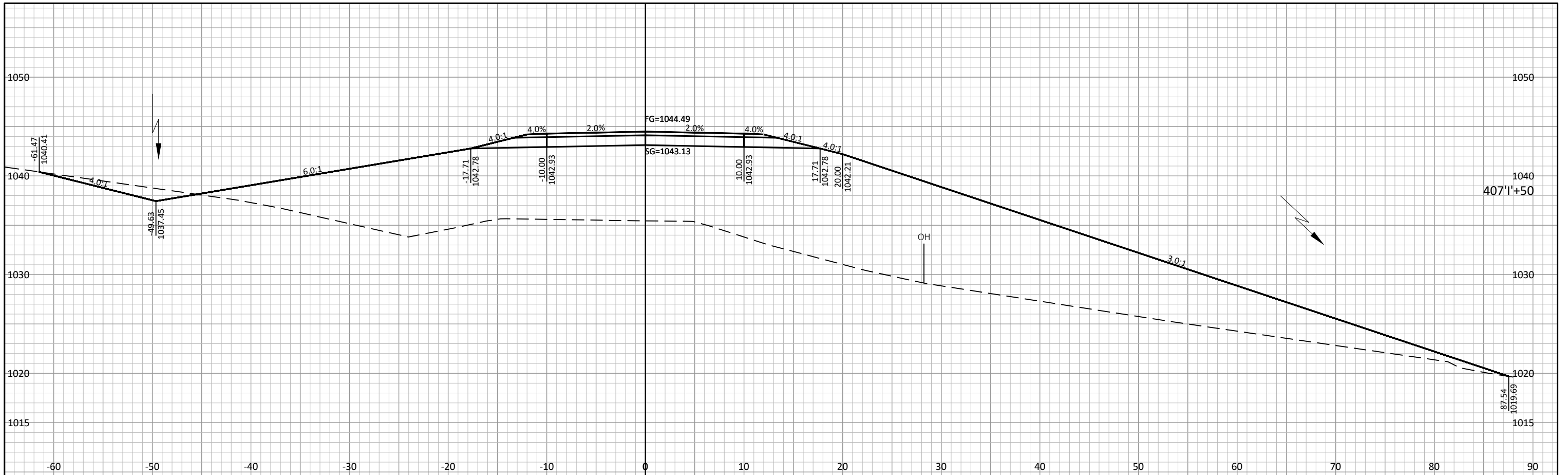


STATION 405'+00
 (1) CULVERT PIPE CORRUGATED STEEL 30-INCH
 (2) APRON ENDWALLS REQ'D
 SKEW = 0°



9
9





9

9

PROJECT NO: 5034-00-72

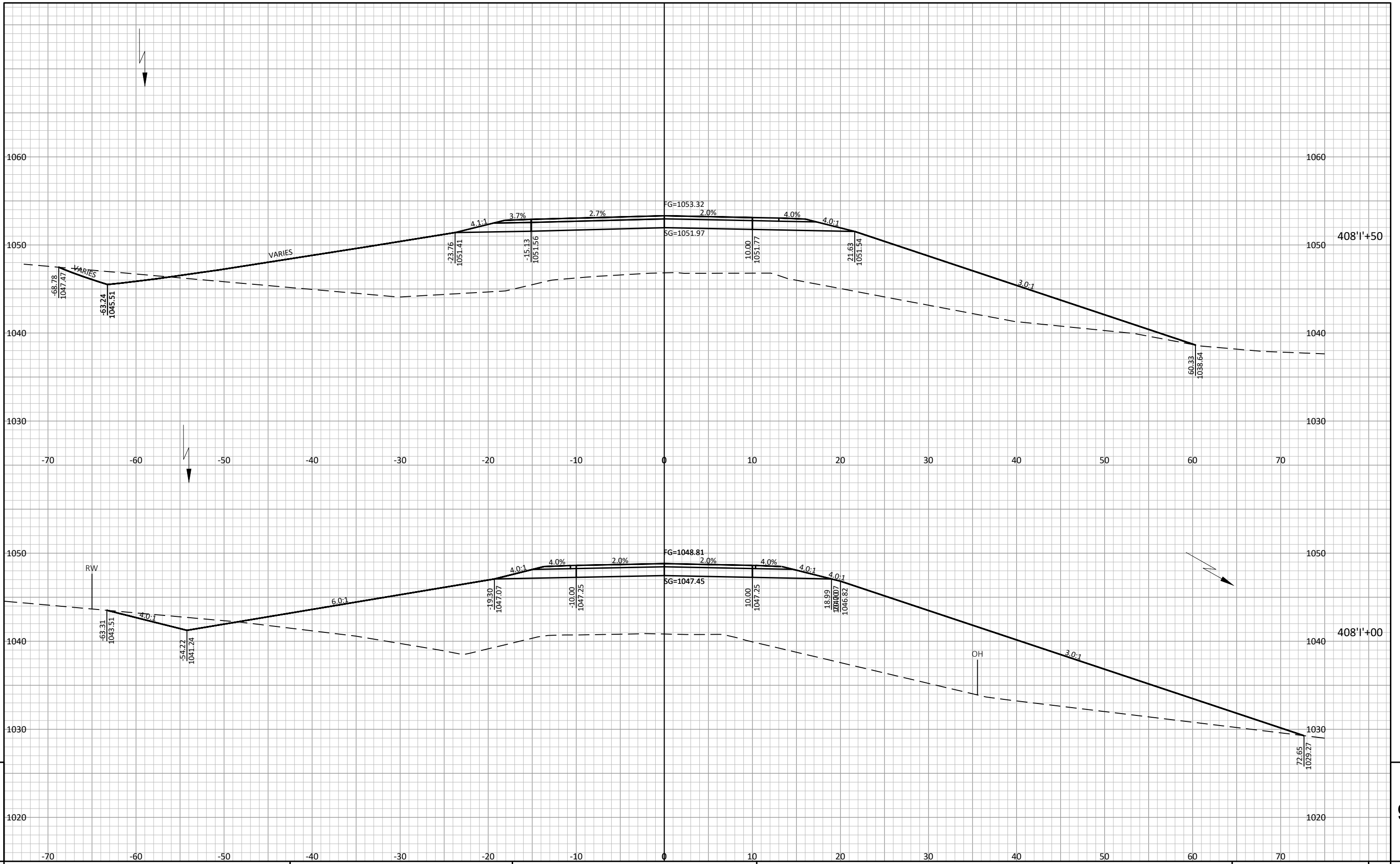
HWY: CTH F

COUNTY: IOWA

CROSS SECTIONS: 'I'-LINE (EAST PECATONICA ROAD)

SHEET

E



9

9

PROJECT NO: 5034-00-72

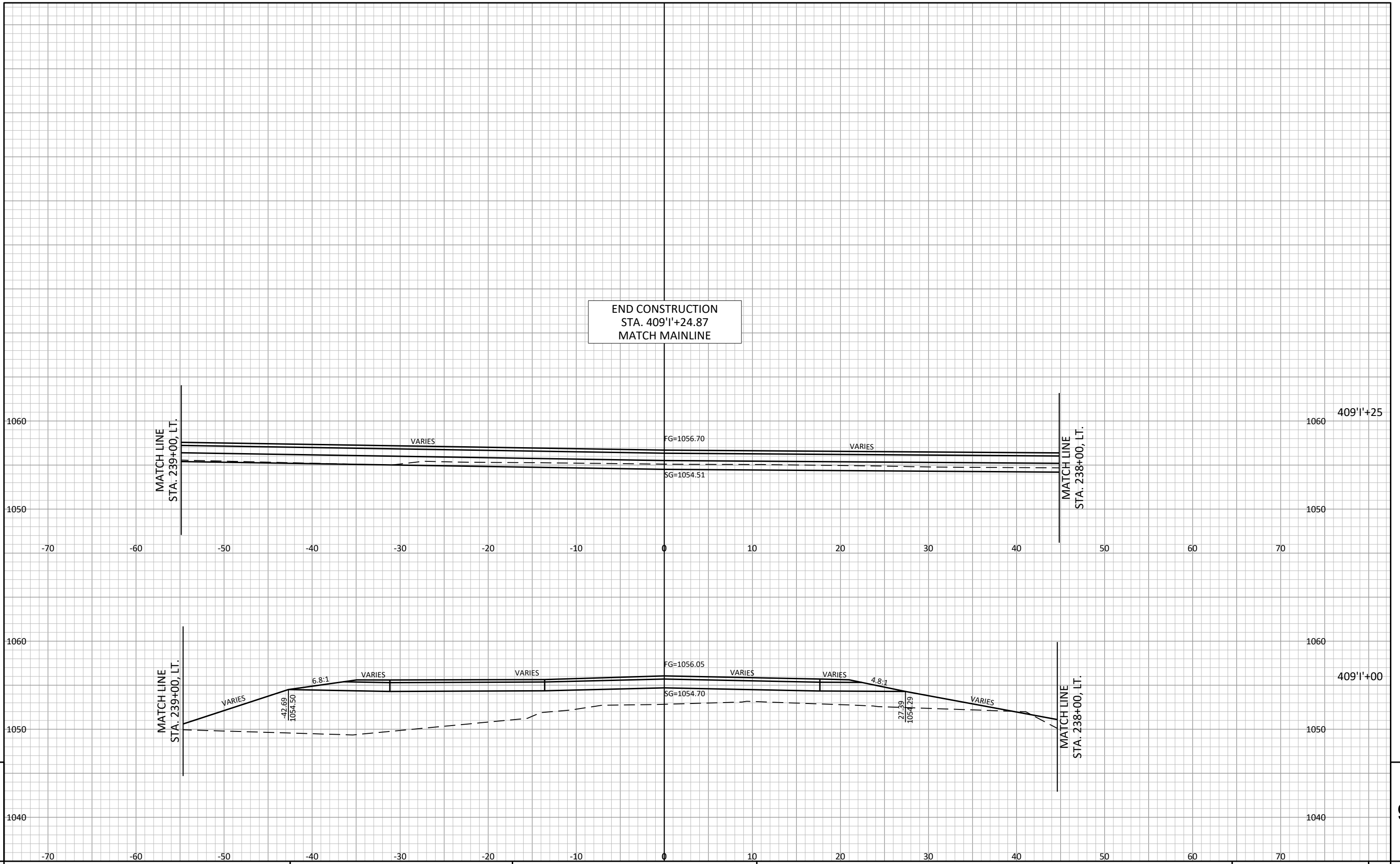
HWY: CTH F

COUNTY: IOWA

CROSS SECTIONS: 'I'-LINE (EAST PECATONICA ROAD)

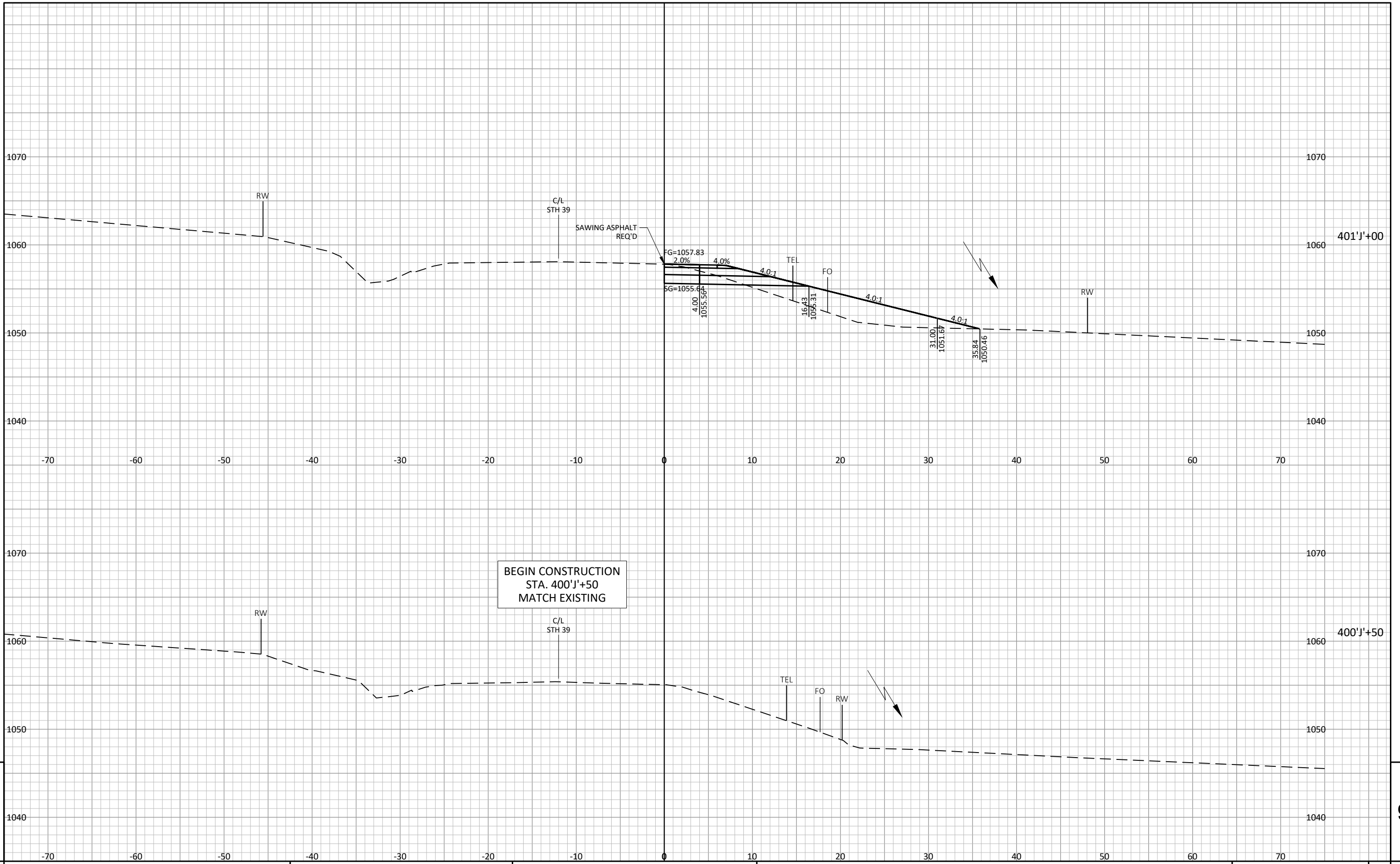
SHEET

E



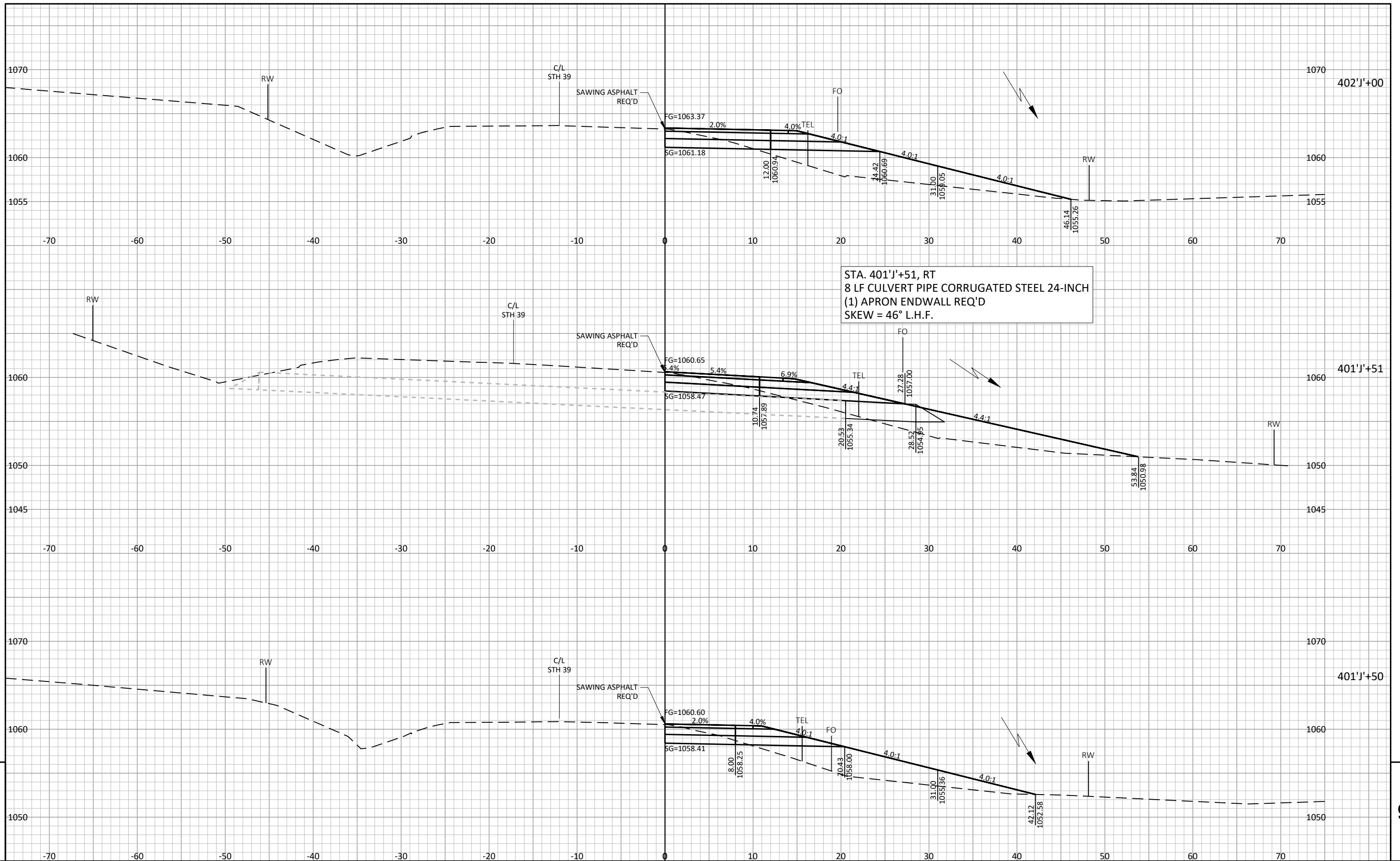
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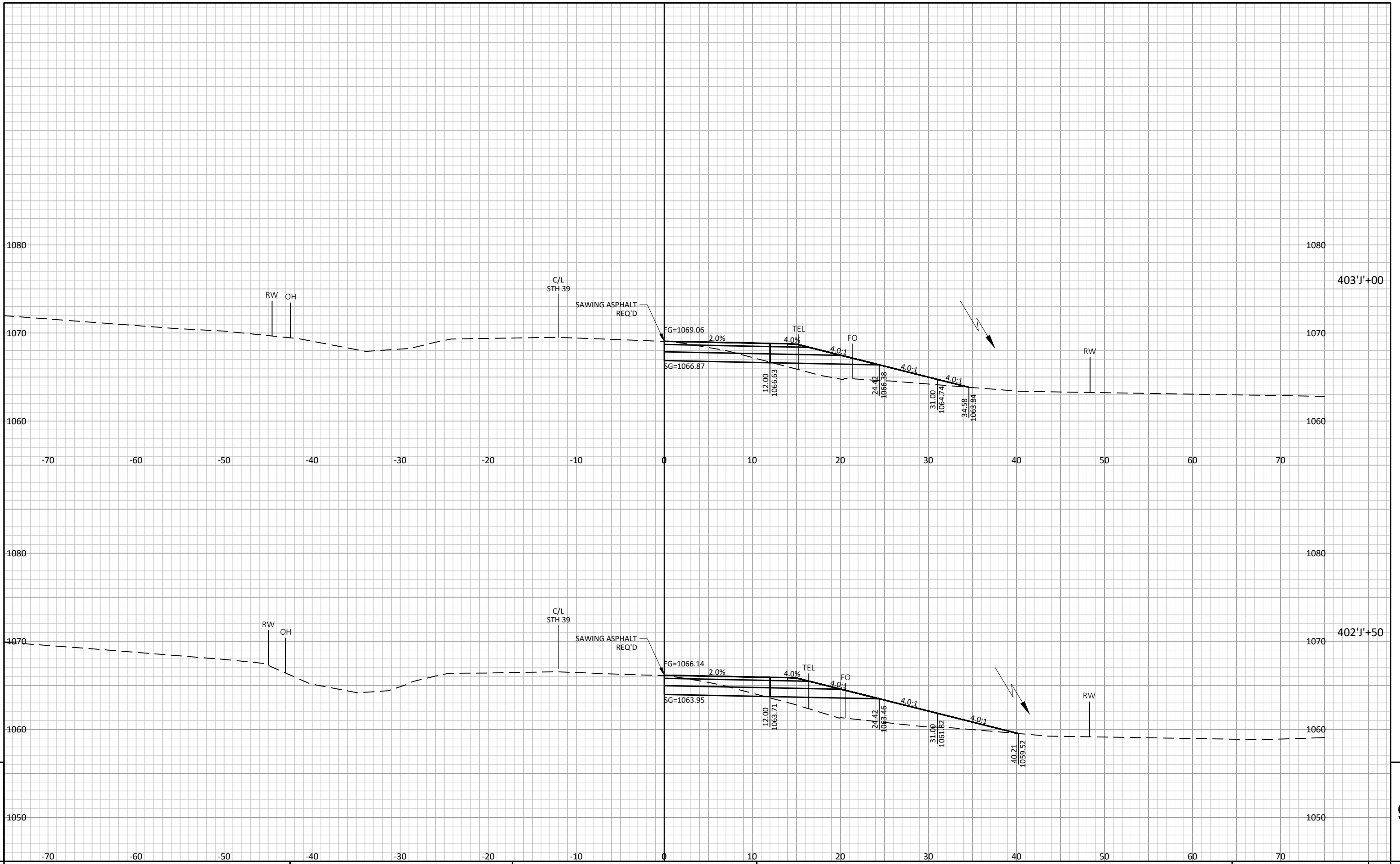
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STA. 401'J'+51, RT
 8 LF CULVERT PIPE CORRUGATED STEEL 24-INCH
 (1) APRON ENDWALL REQ'D
 SKEW = 46° L.H.F.

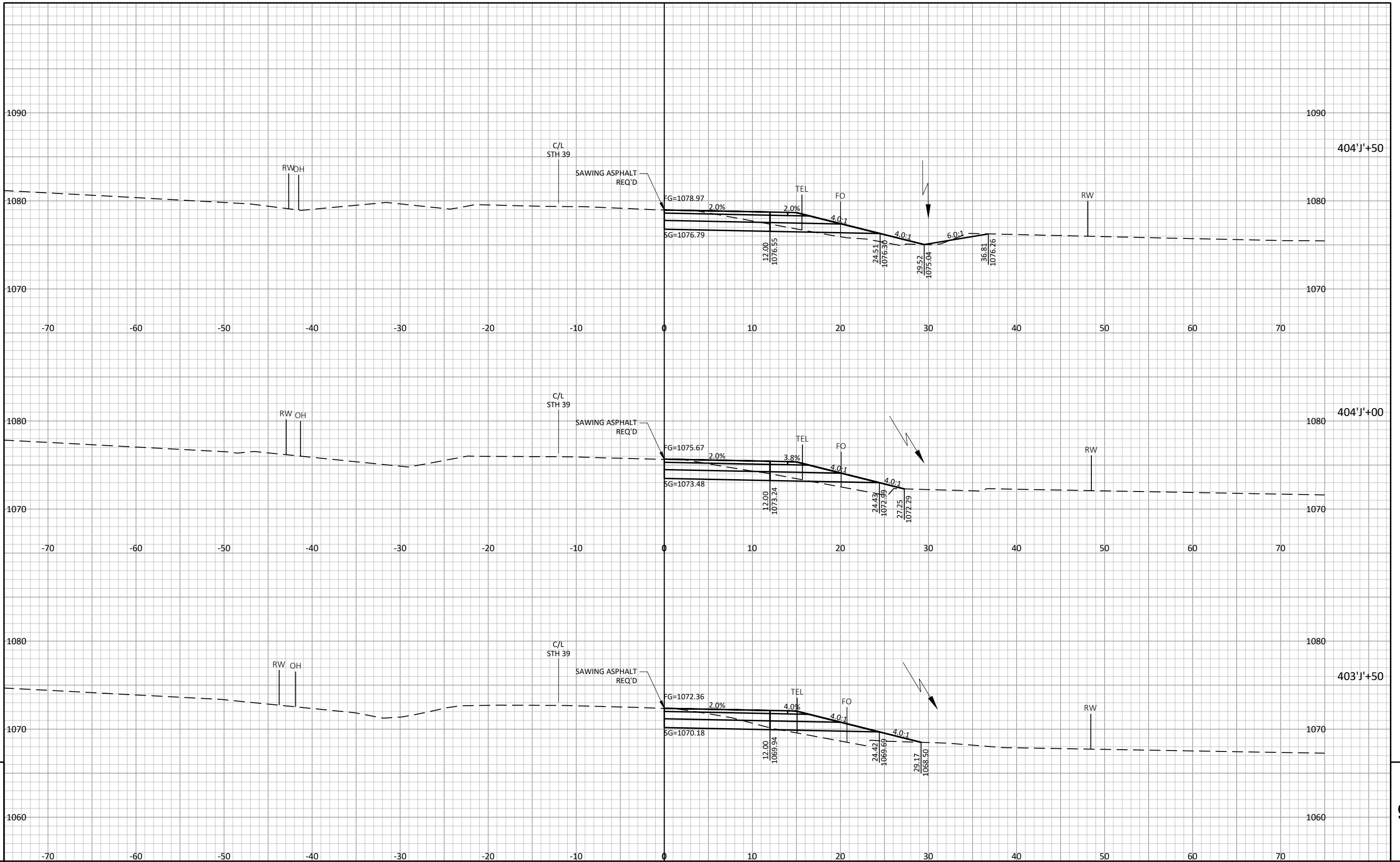
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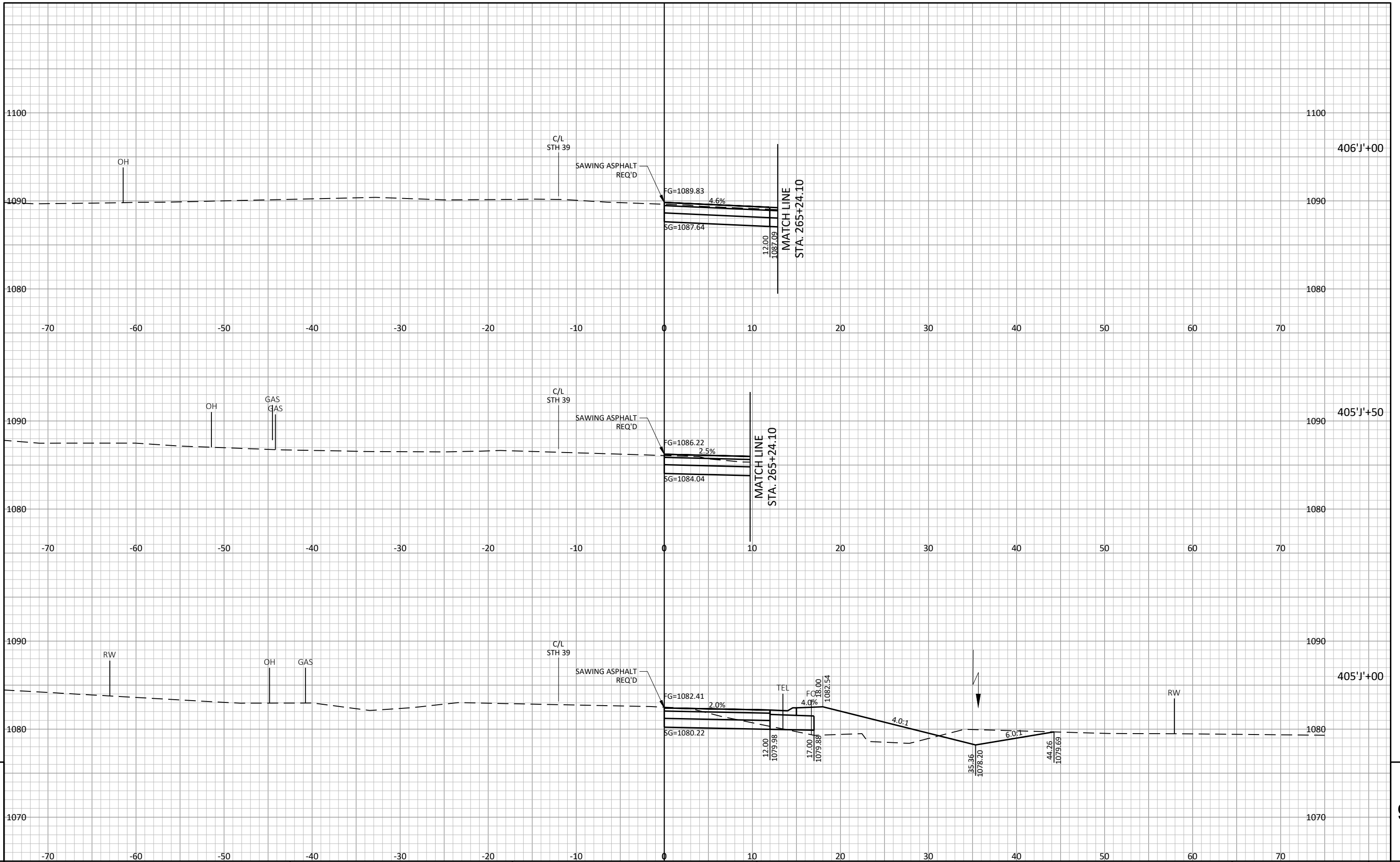
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PROJECT NO: 5034-00-72

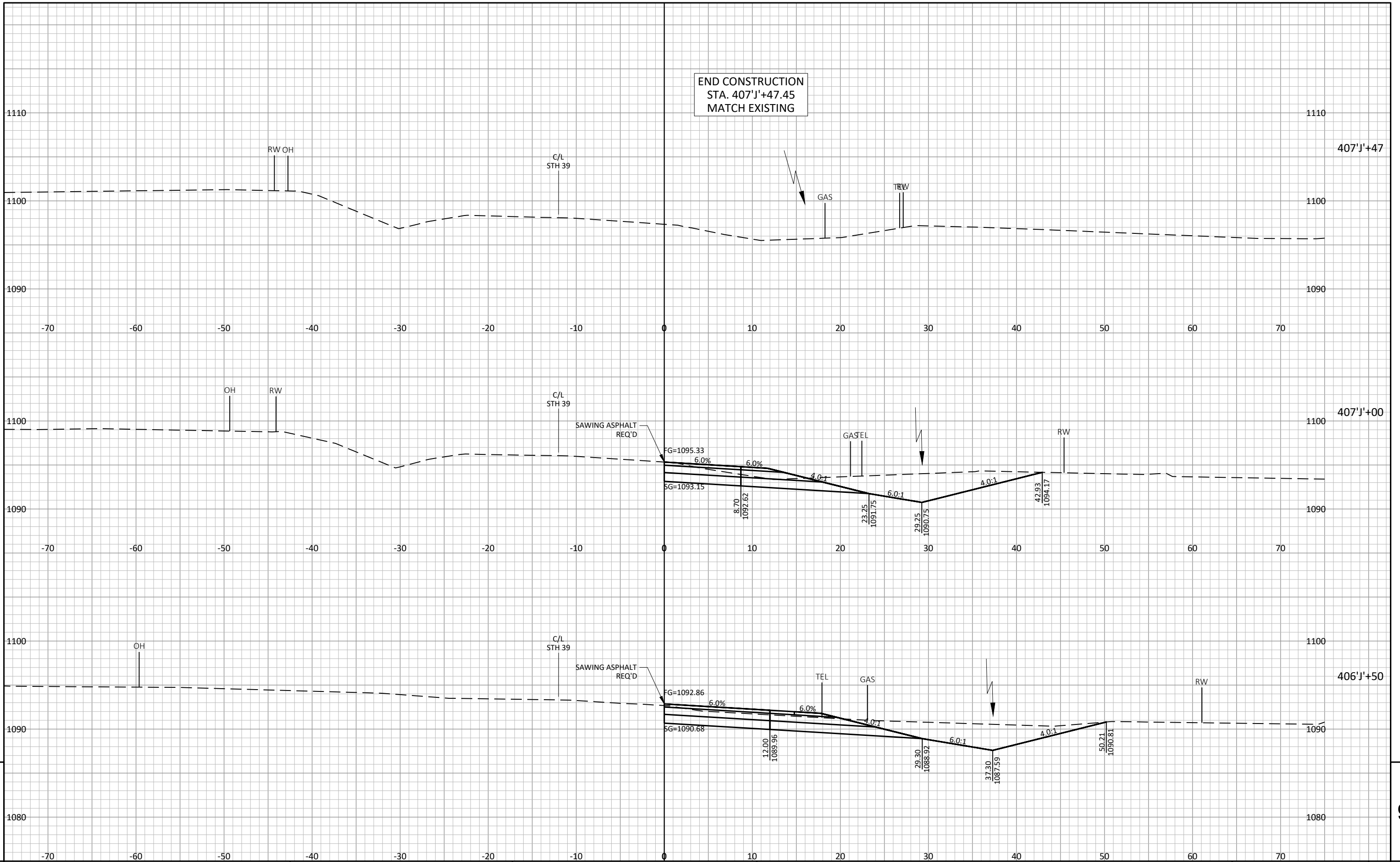
HWY: CTH F

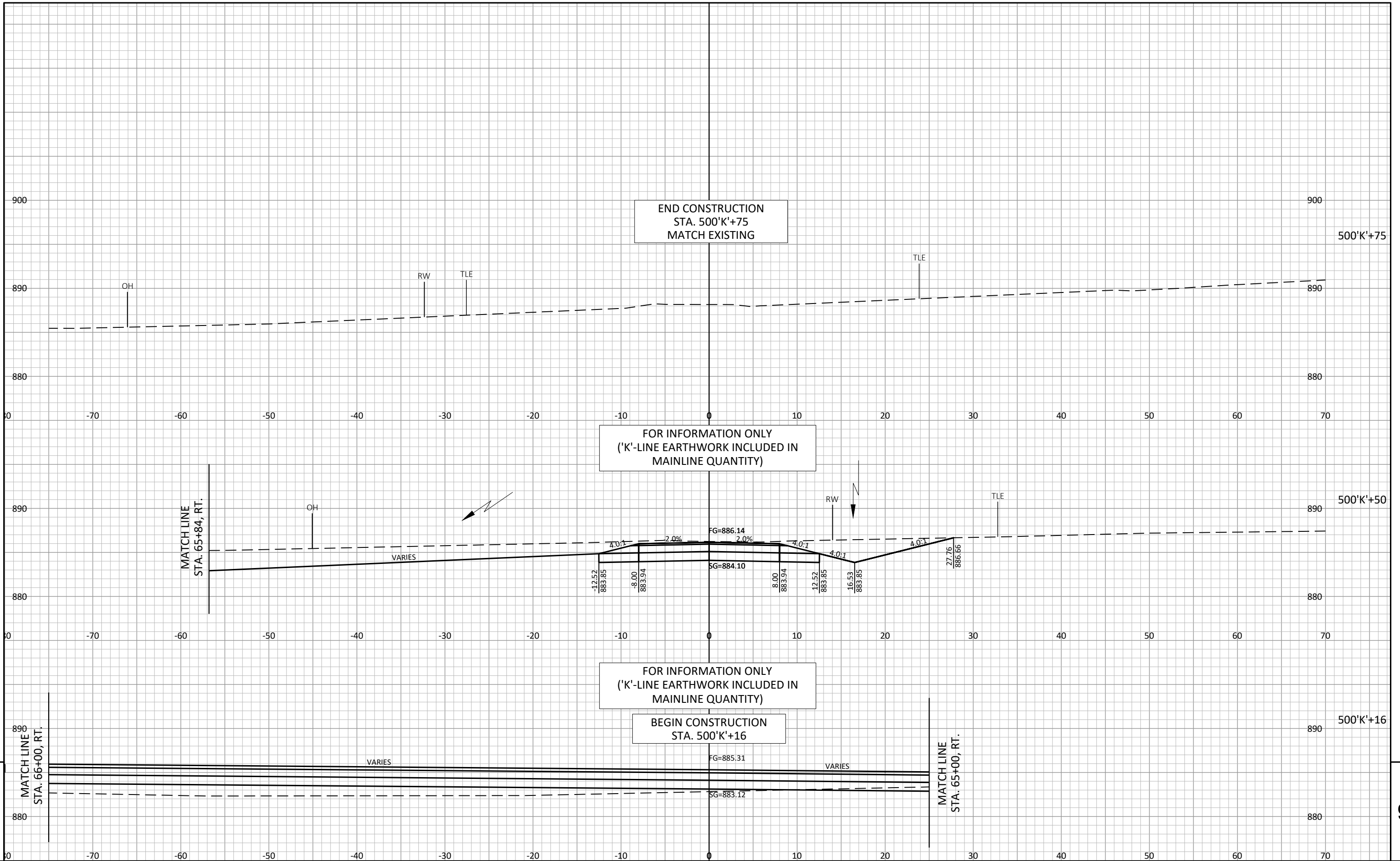
COUNTY: IOWA

CROSS SECTIONS: 'J'-LINE (STH 39)

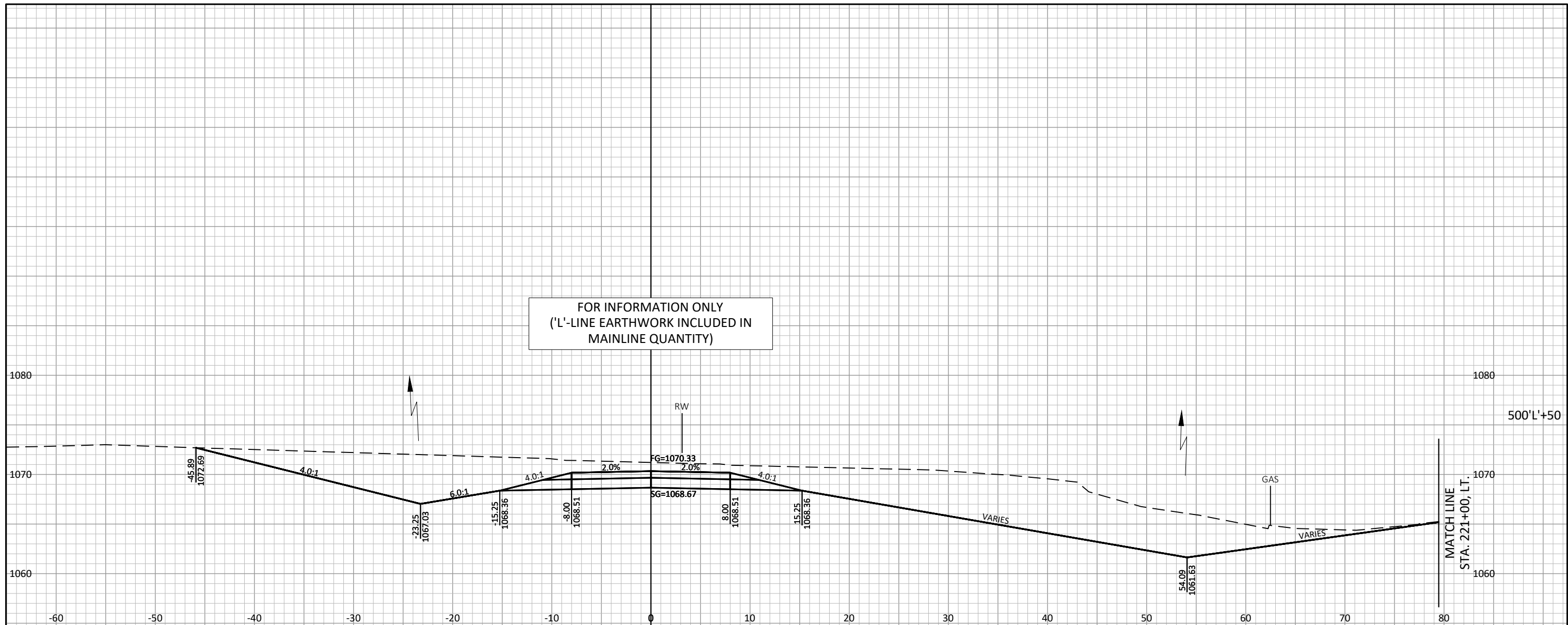
SHEET

E

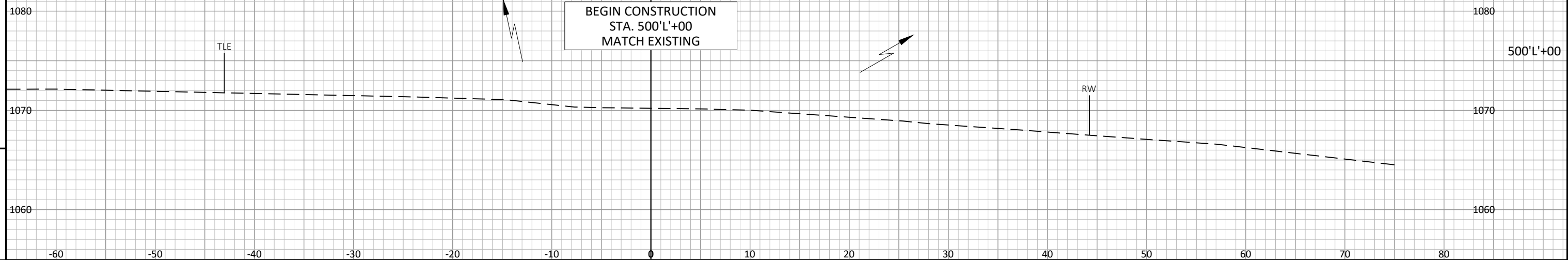


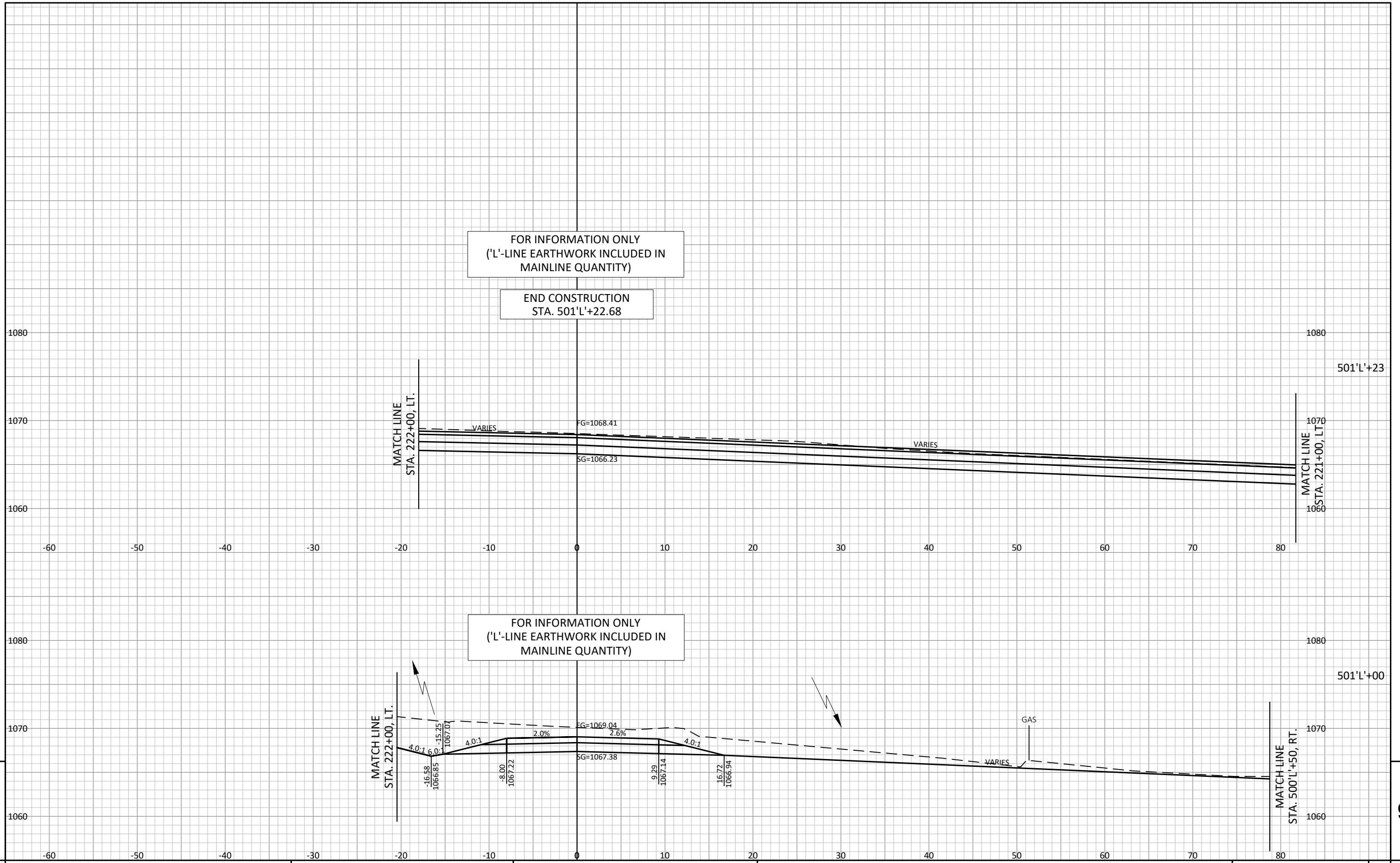


FOR INFORMATION ONLY
(L'-LINE EARTHWORK INCLUDED IN
MAINLINE QUANTITY)



BEGIN CONSTRUCTION
STA. 500'L'+00
MATCH EXISTING





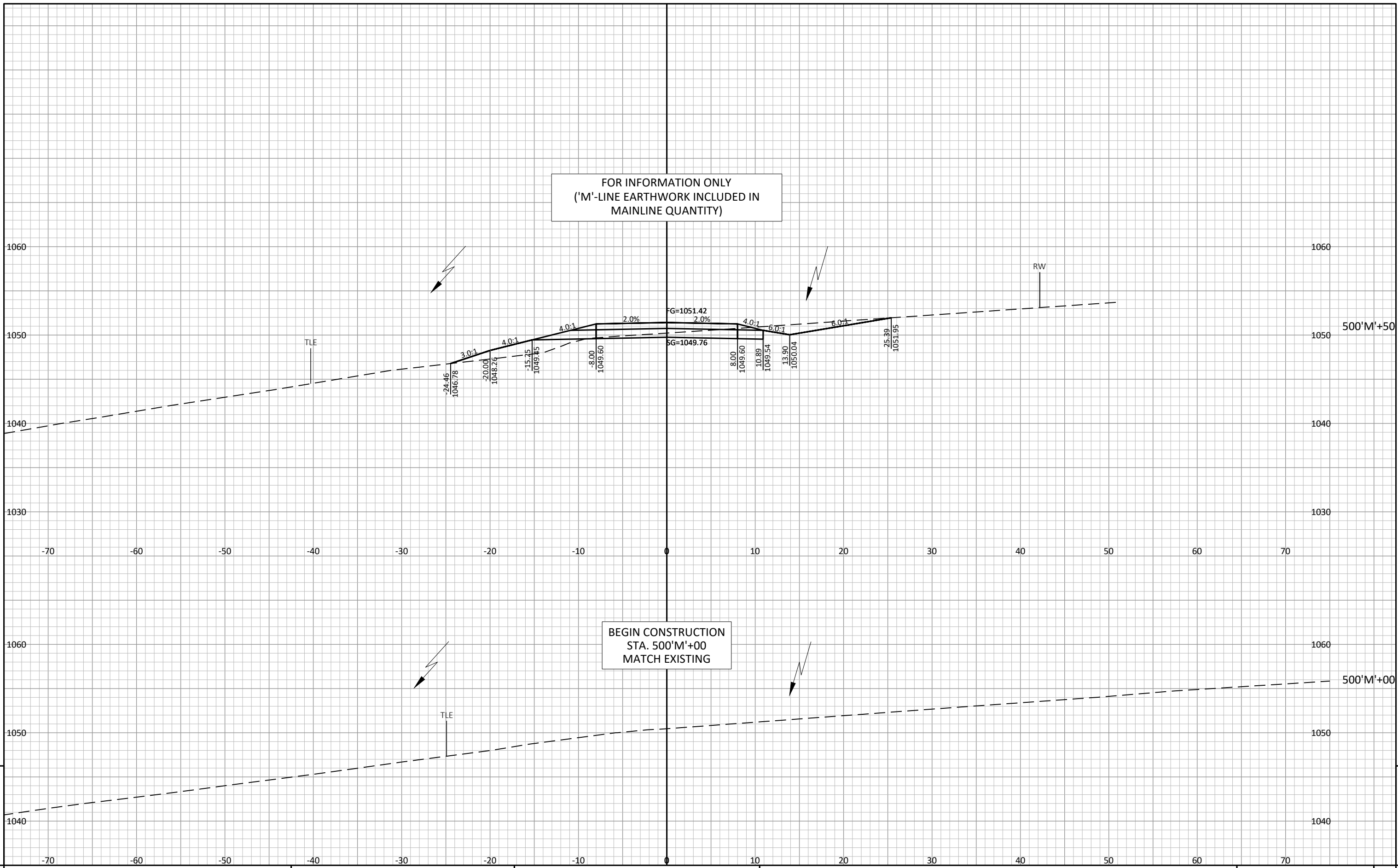
FOR INFORMATION ONLY
('L'-LINE EARTHWORK INCLUDED IN
MAINLINE QUANTITY)

END CONSTRUCTION
STA. 501'L'+22.68

FOR INFORMATION ONLY
('L'-LINE EARTHWORK INCLUDED IN
MAINLINE QUANTITY)

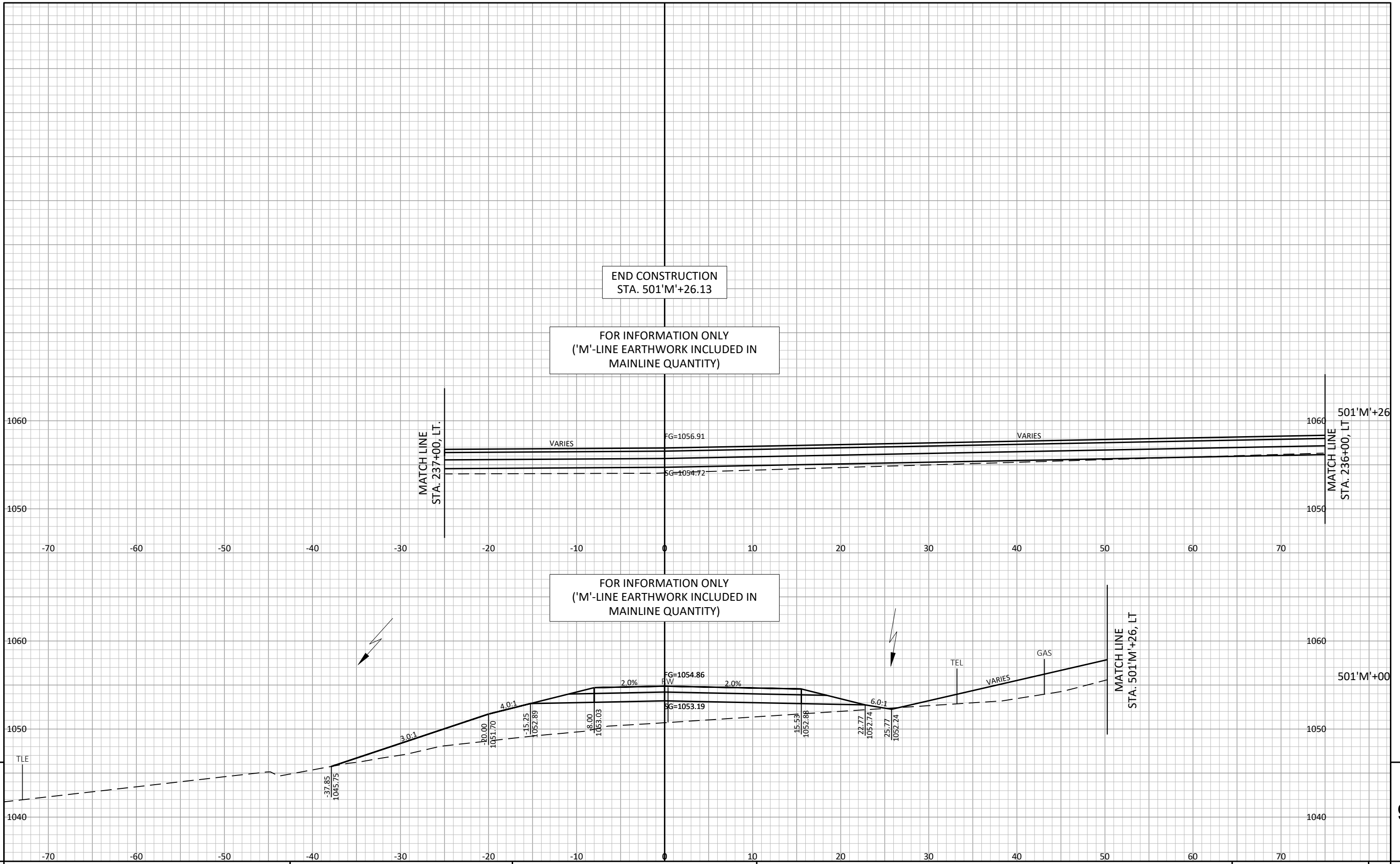
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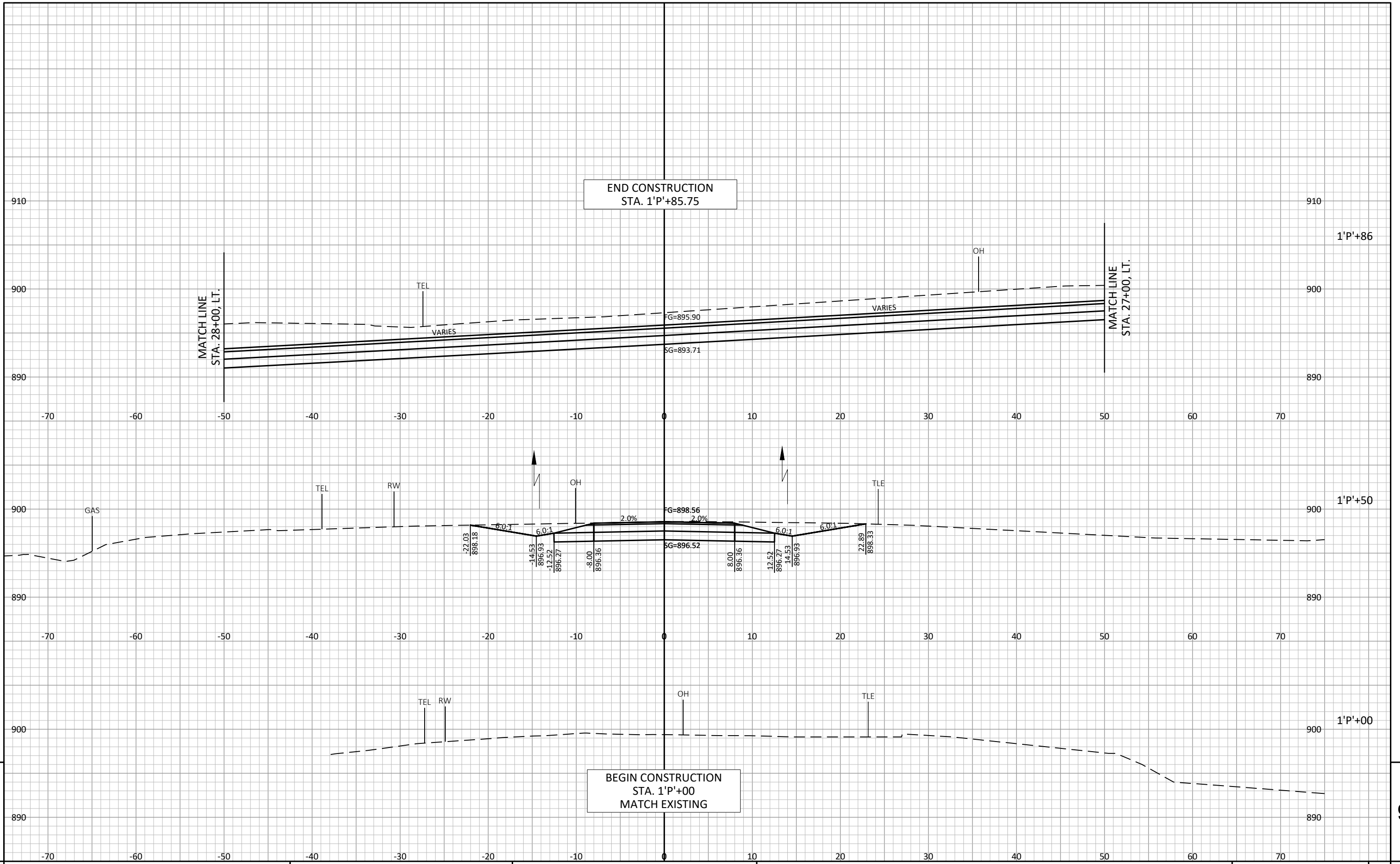
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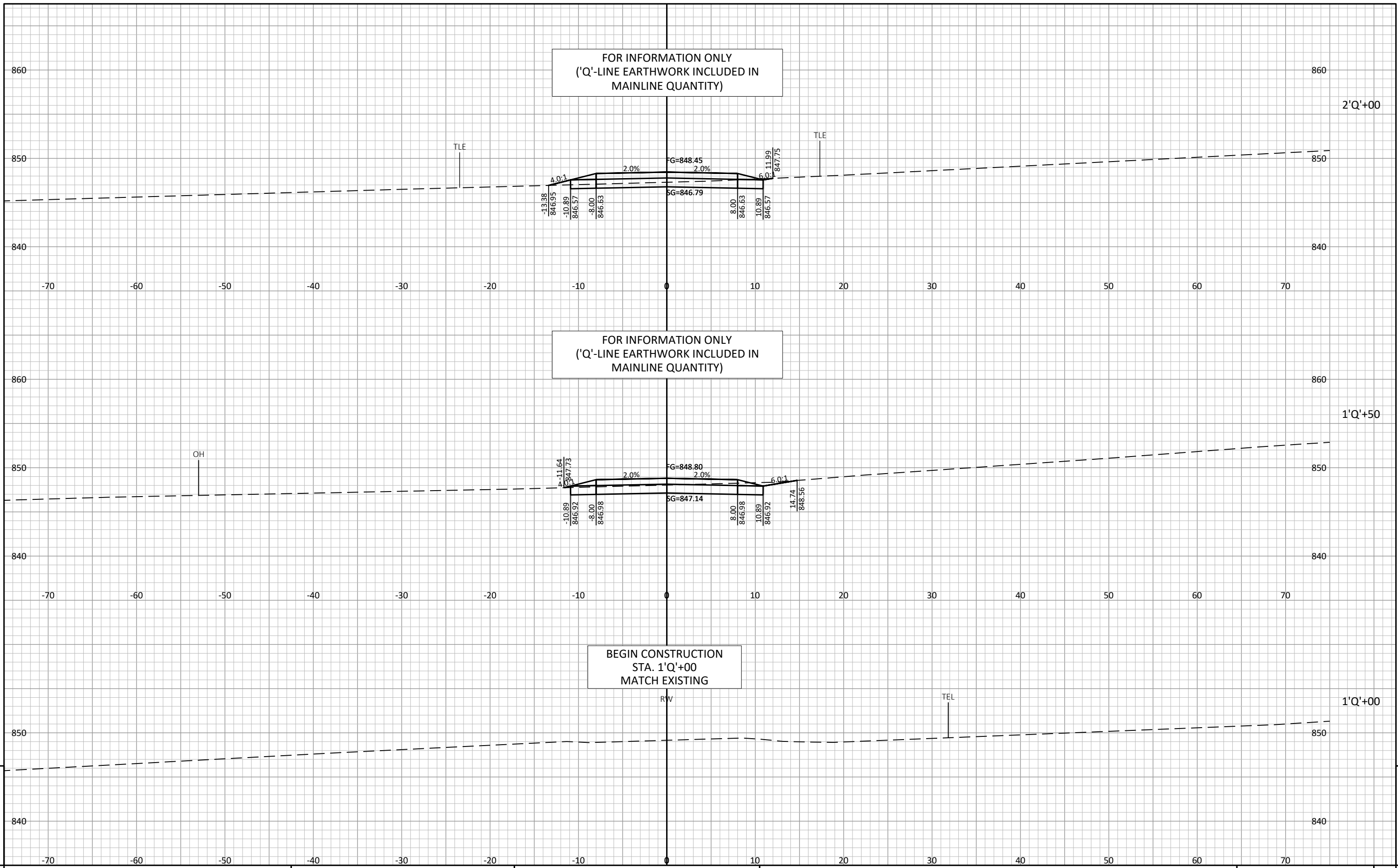
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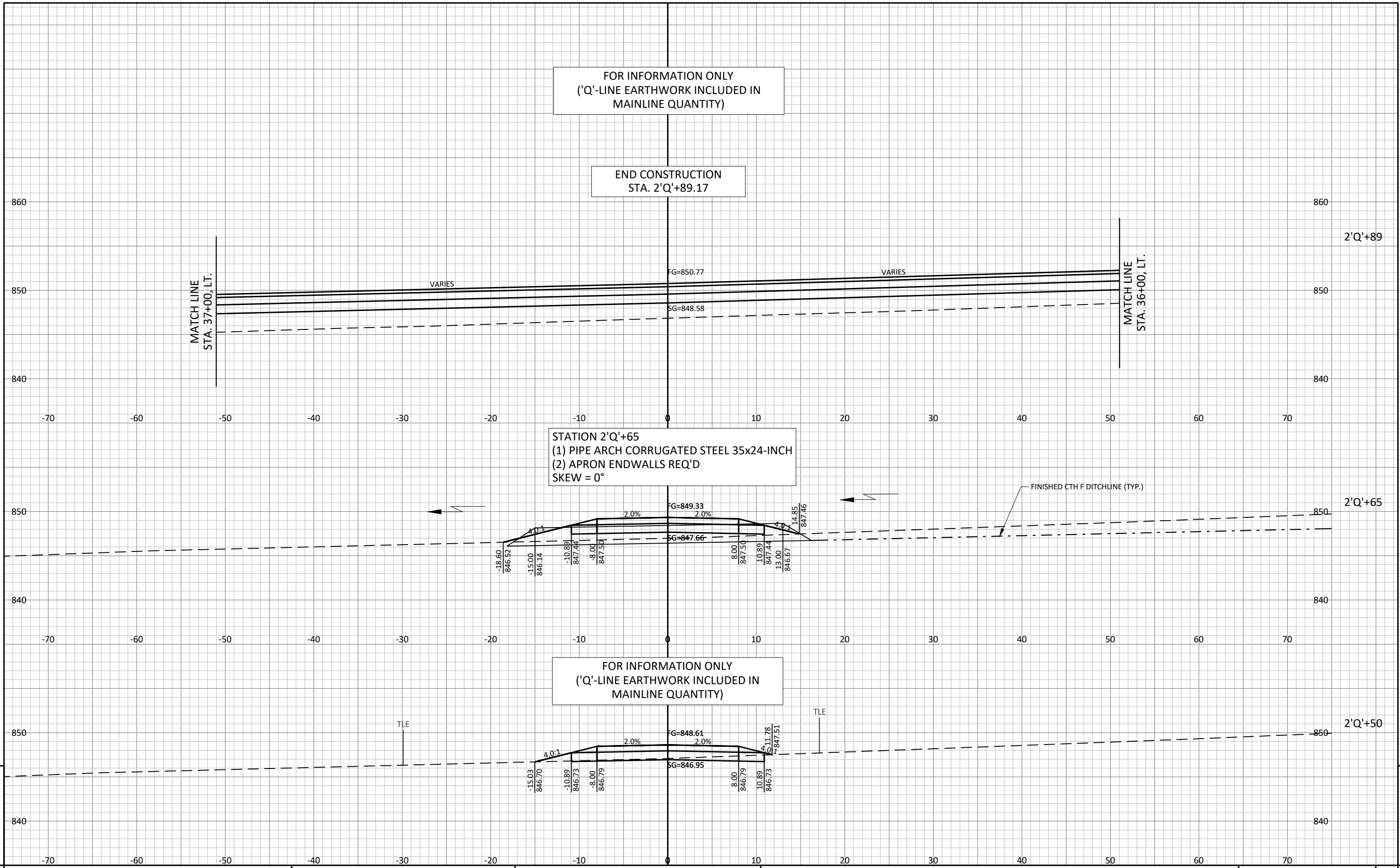
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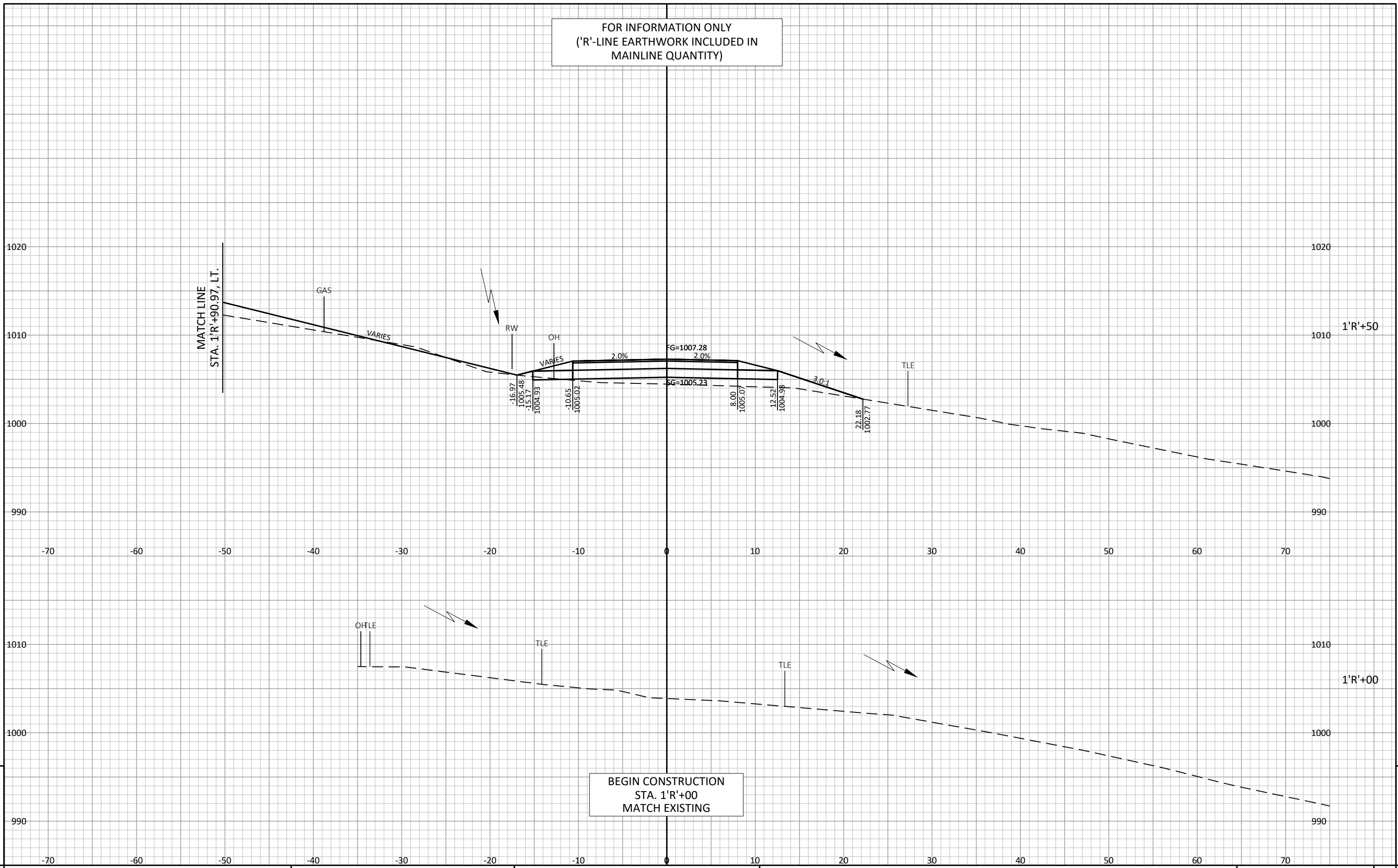
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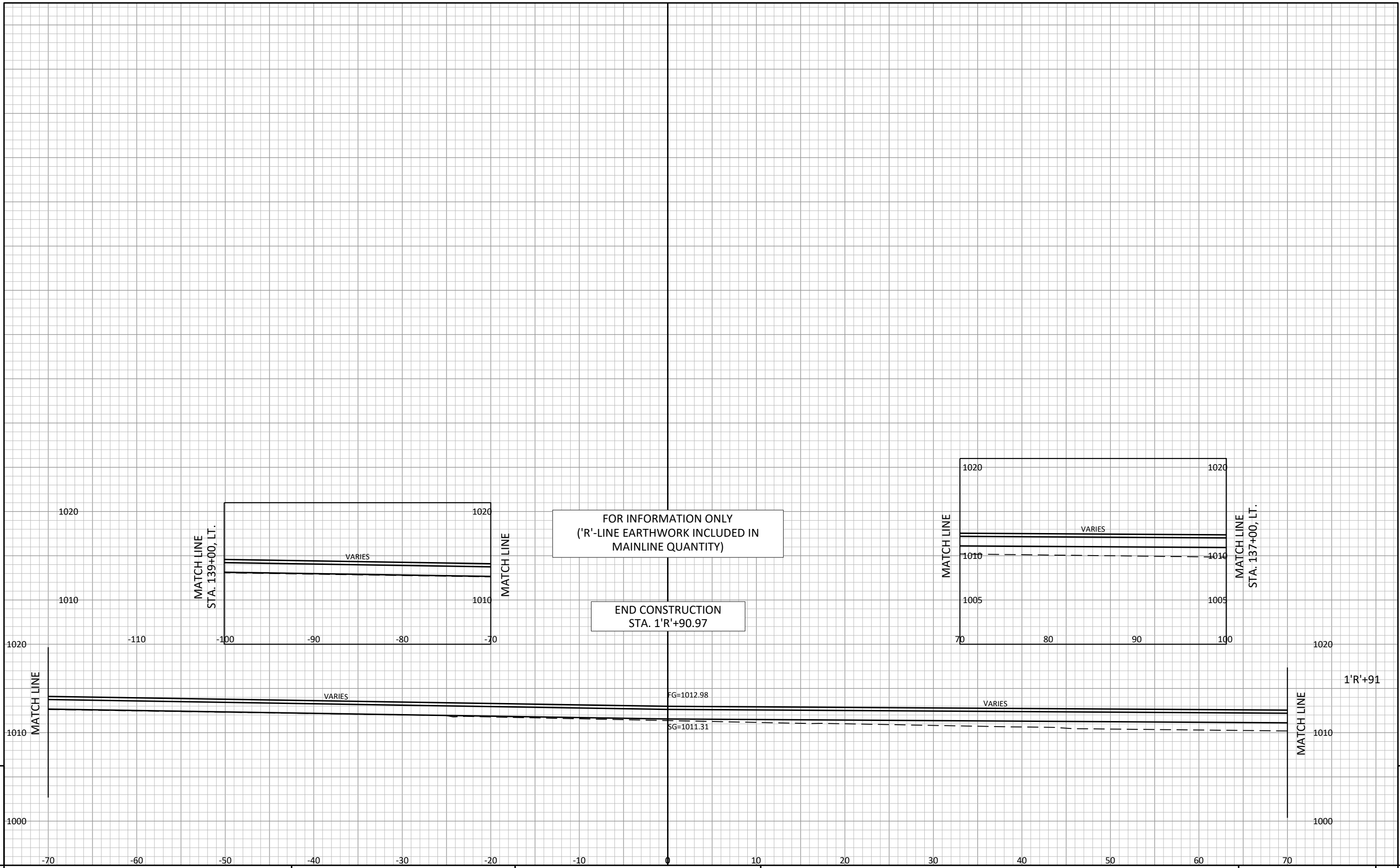
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FOR INFORMATION ONLY
(R'-LINE EARTHWORK INCLUDED IN
MAINLINE QUANTITY)



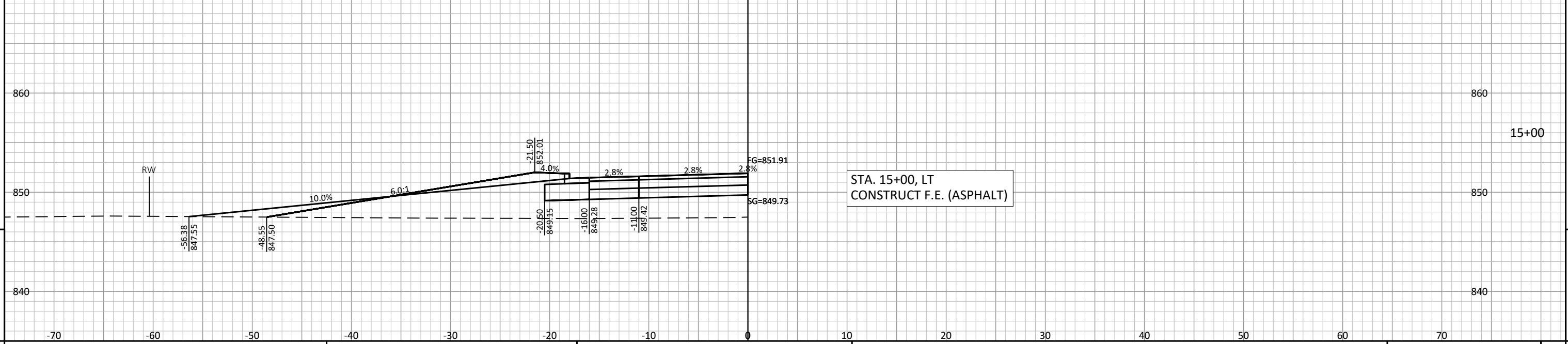
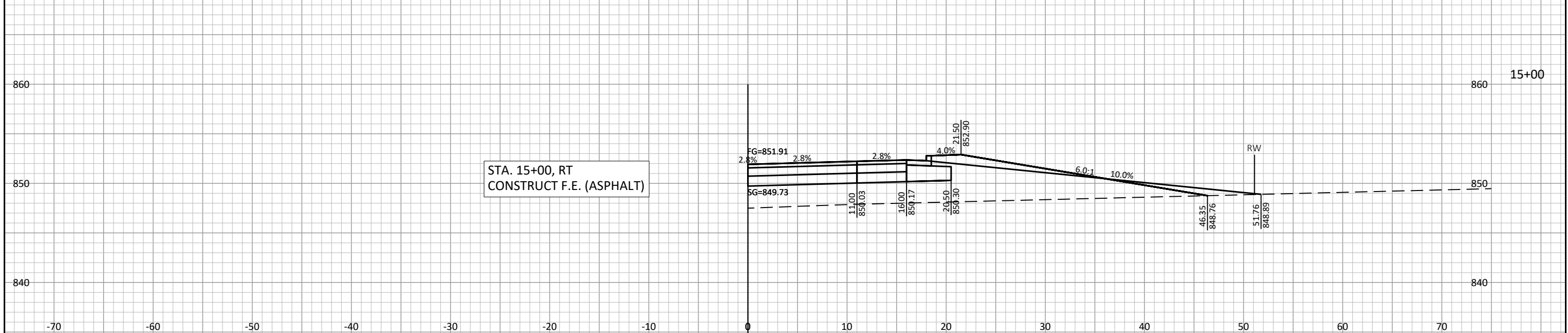
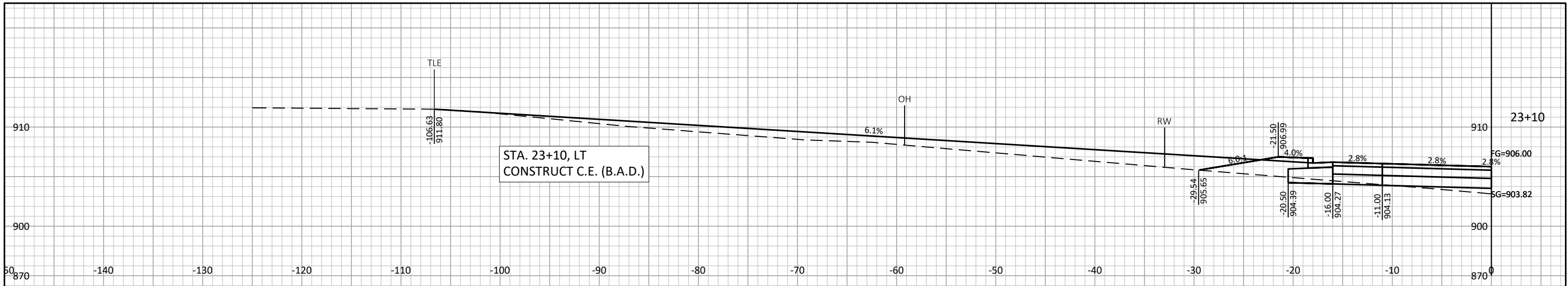
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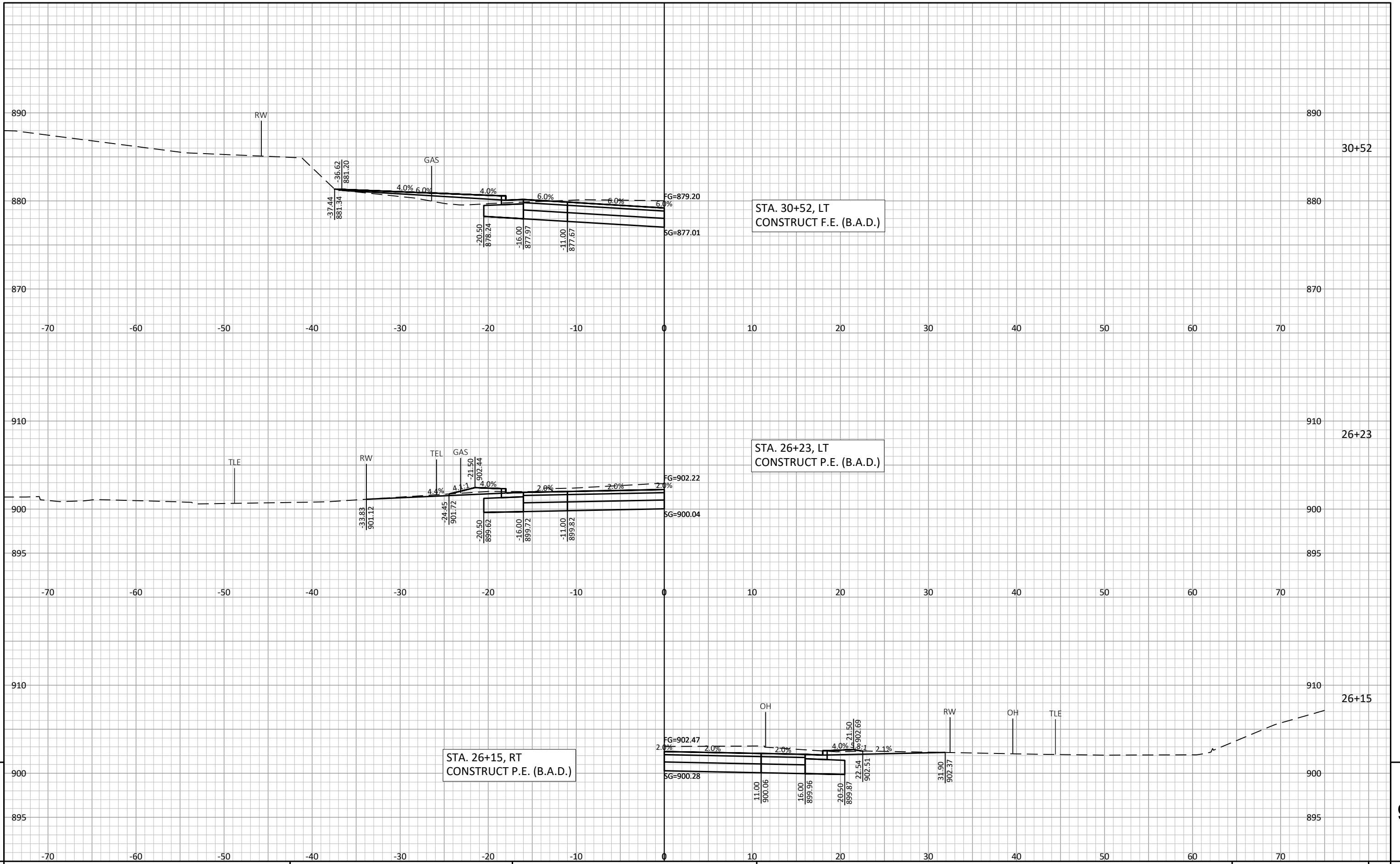
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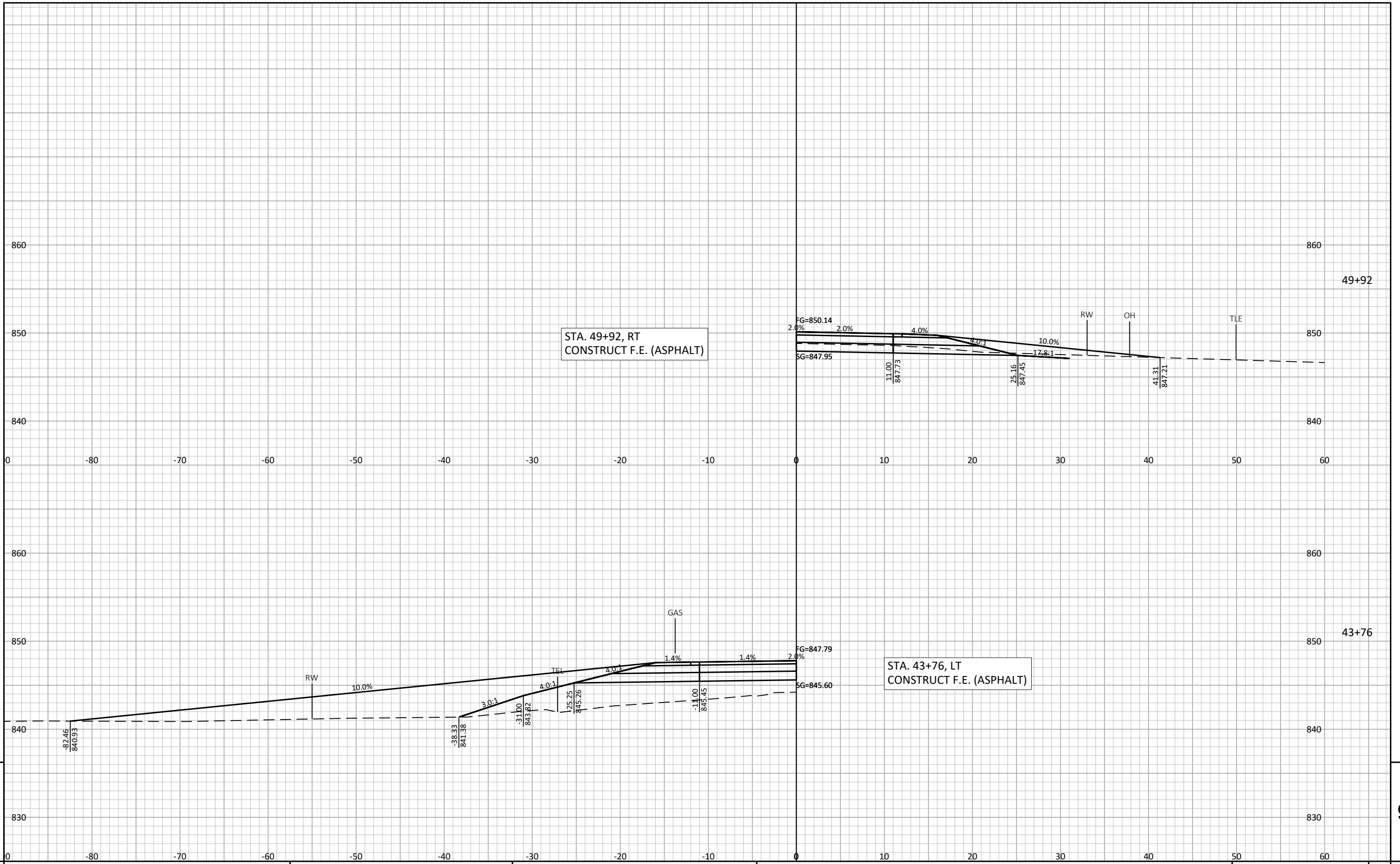


FOR INFORMATION ONLY
('R'-LINE EARTHWORK INCLUDED IN
MAINLINE QUANTITY)

END CONSTRUCTION
STA. 1'R'+90.97

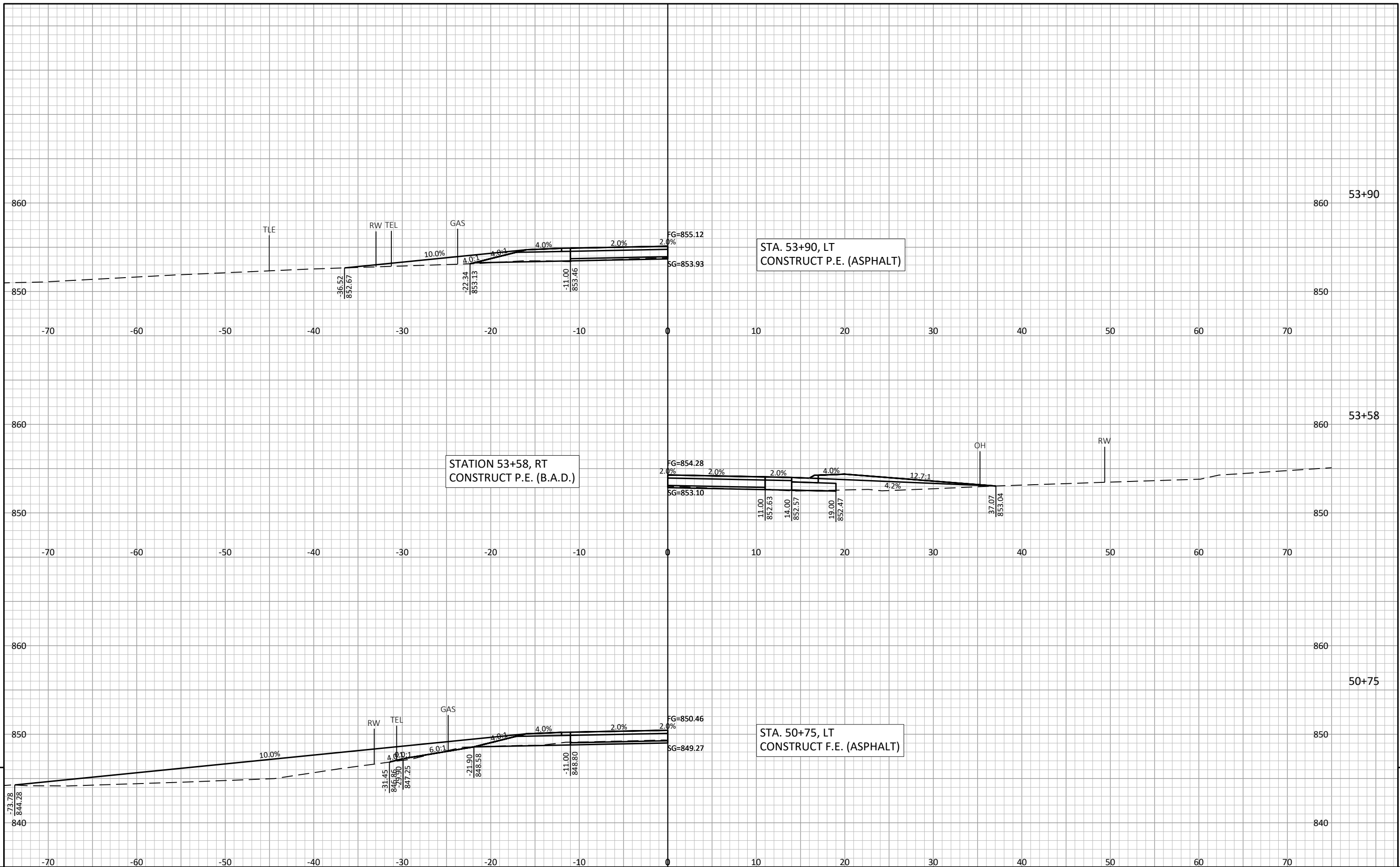






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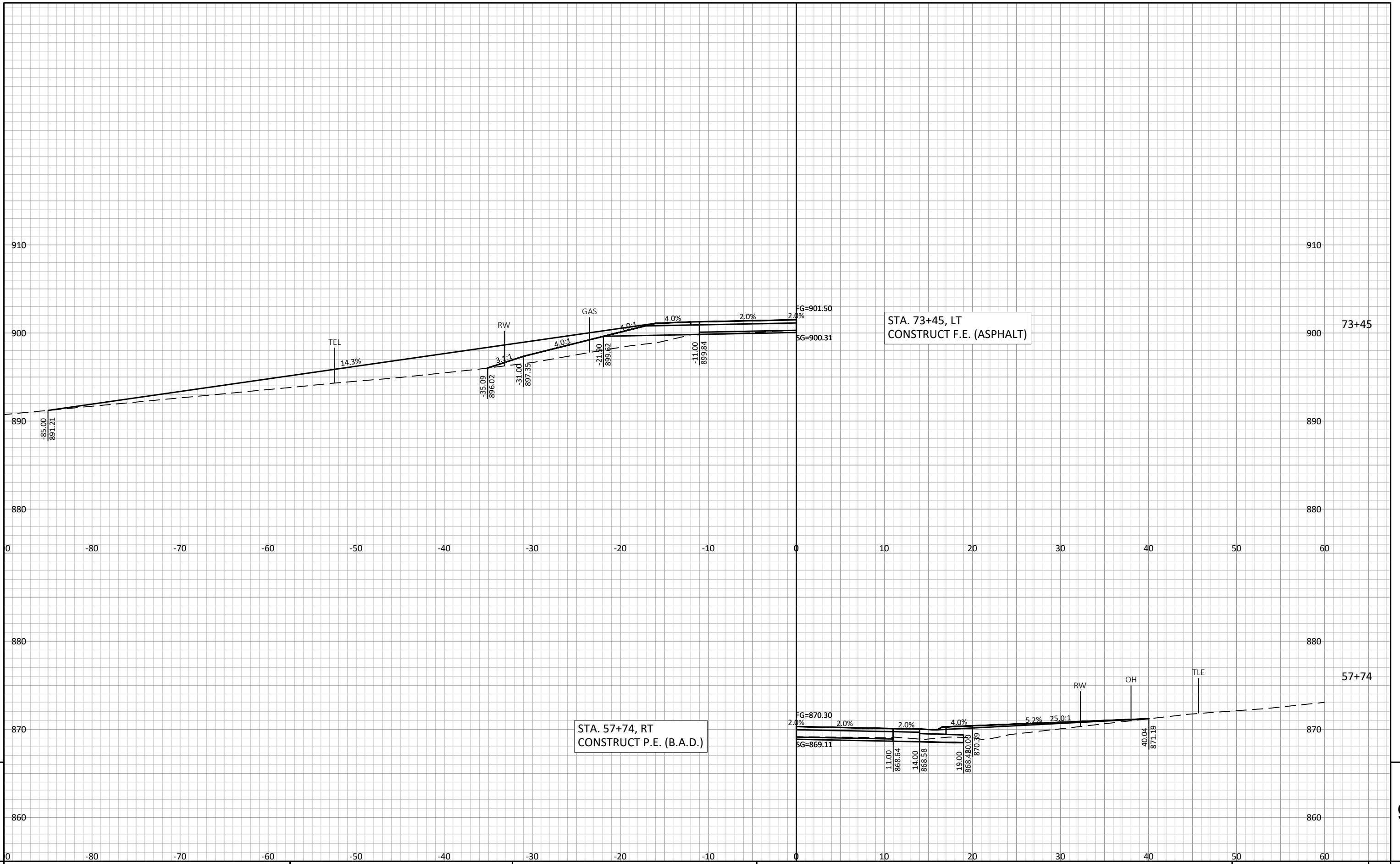
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STA. 53+90, LT
CONSTRUCT P.E. (ASPHALT)

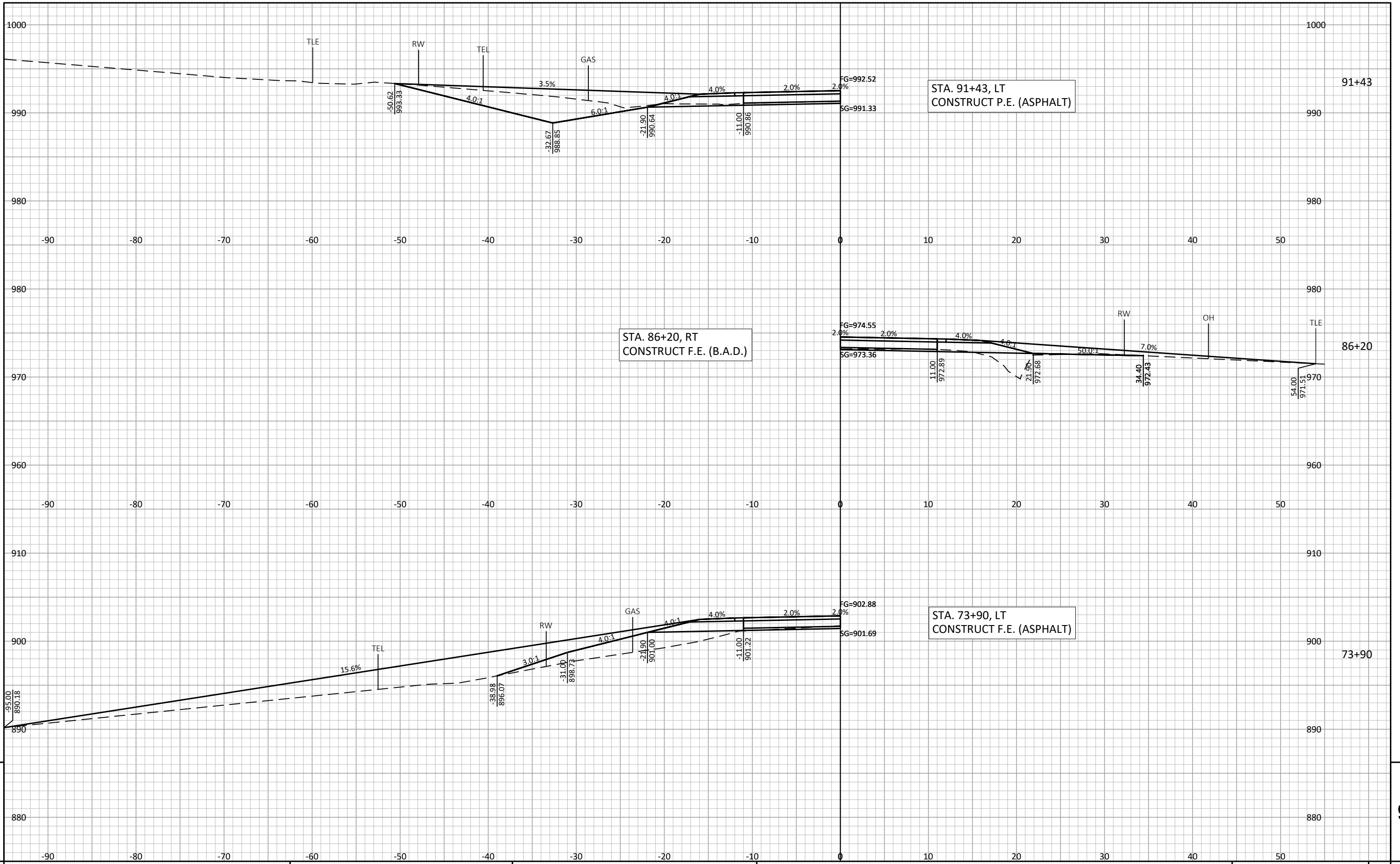
STATION 53+58, RT
CONSTRUCT P.E. (B.A.D.)

STA. 50+75, LT
CONSTRUCT F.E. (ASPHALT)



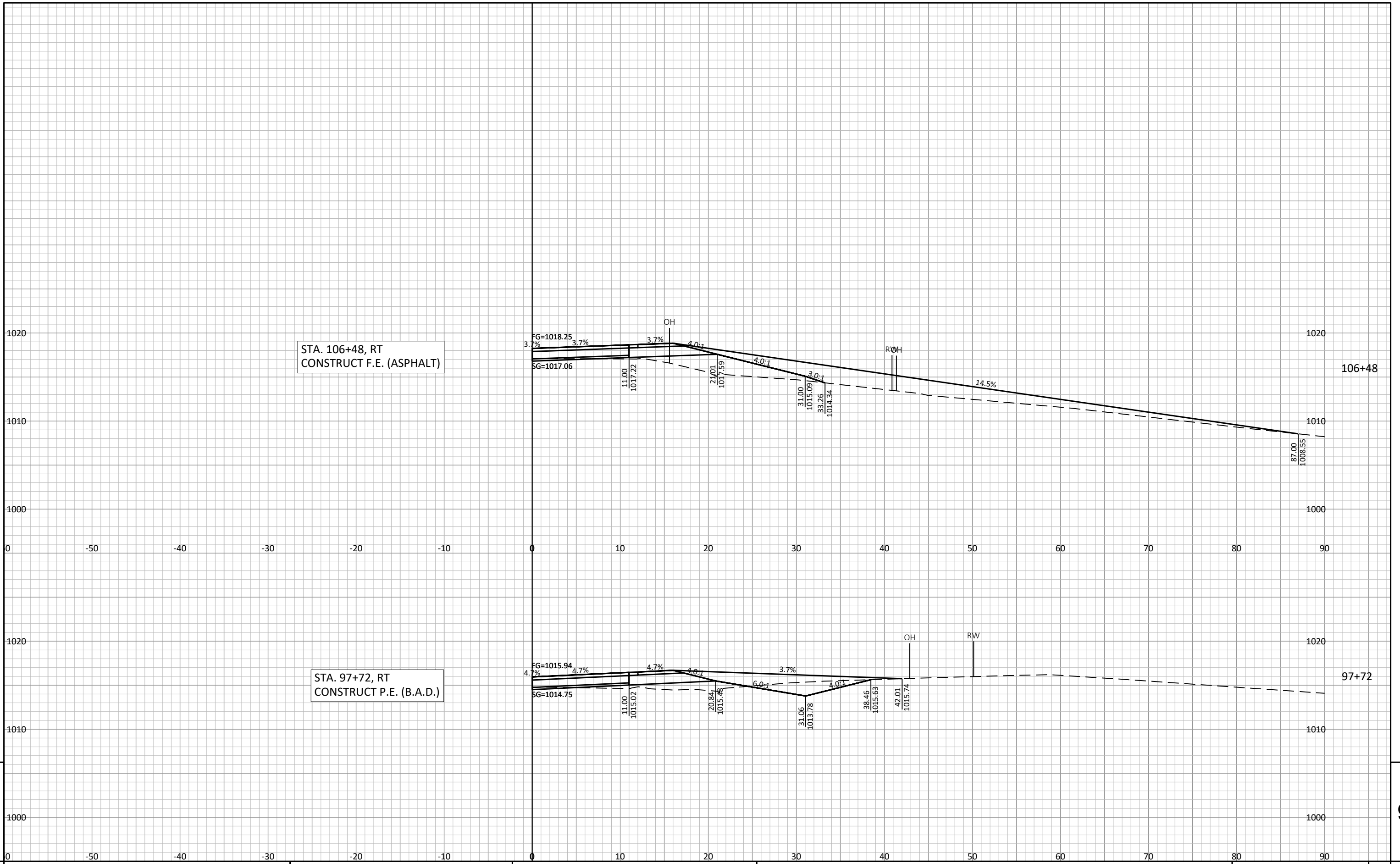
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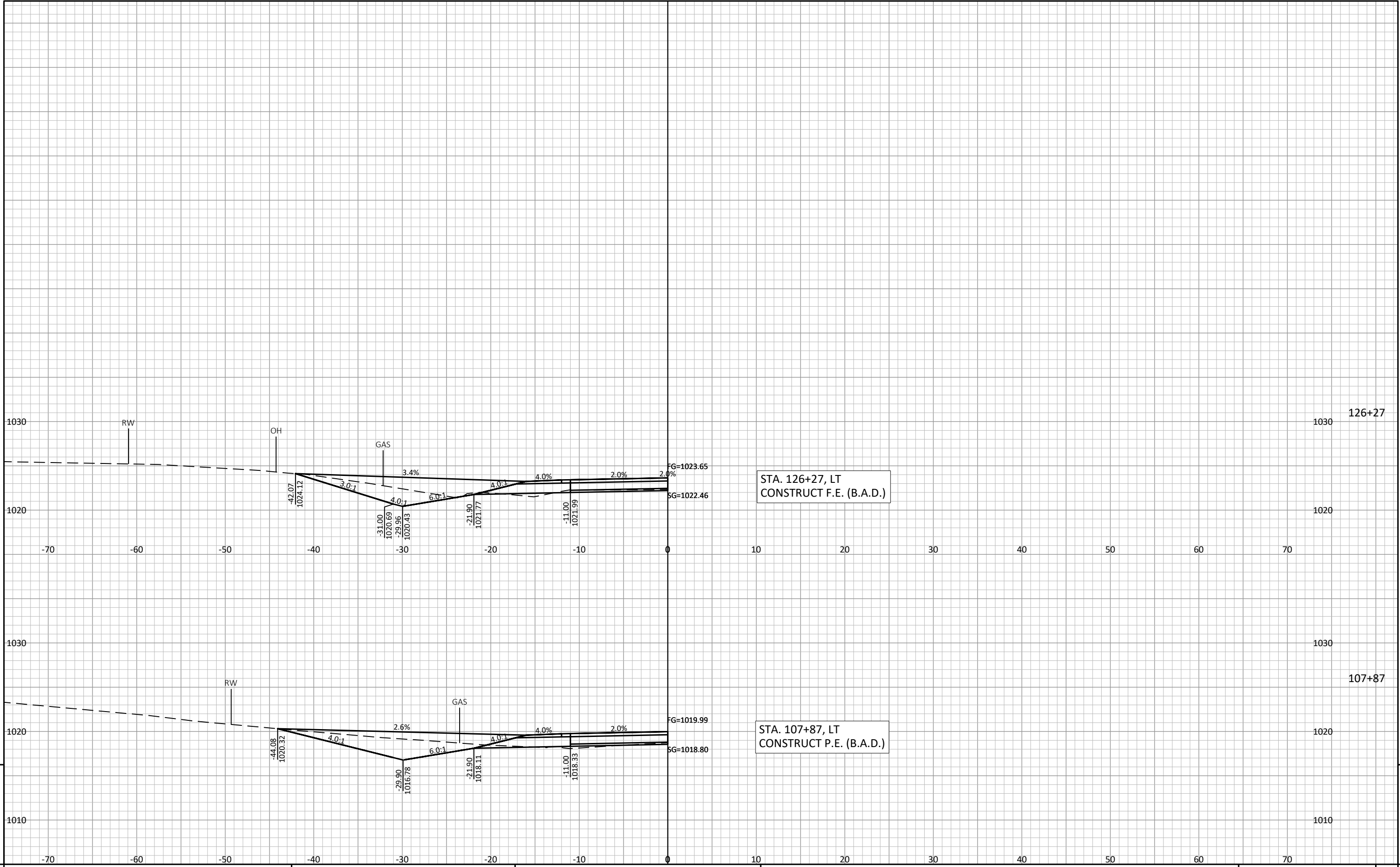


STA. 106+48, RT
CONSTRUCT F.E. (ASPHALT)

STA. 97+72, RT
CONSTRUCT P.E. (B.A.D.)

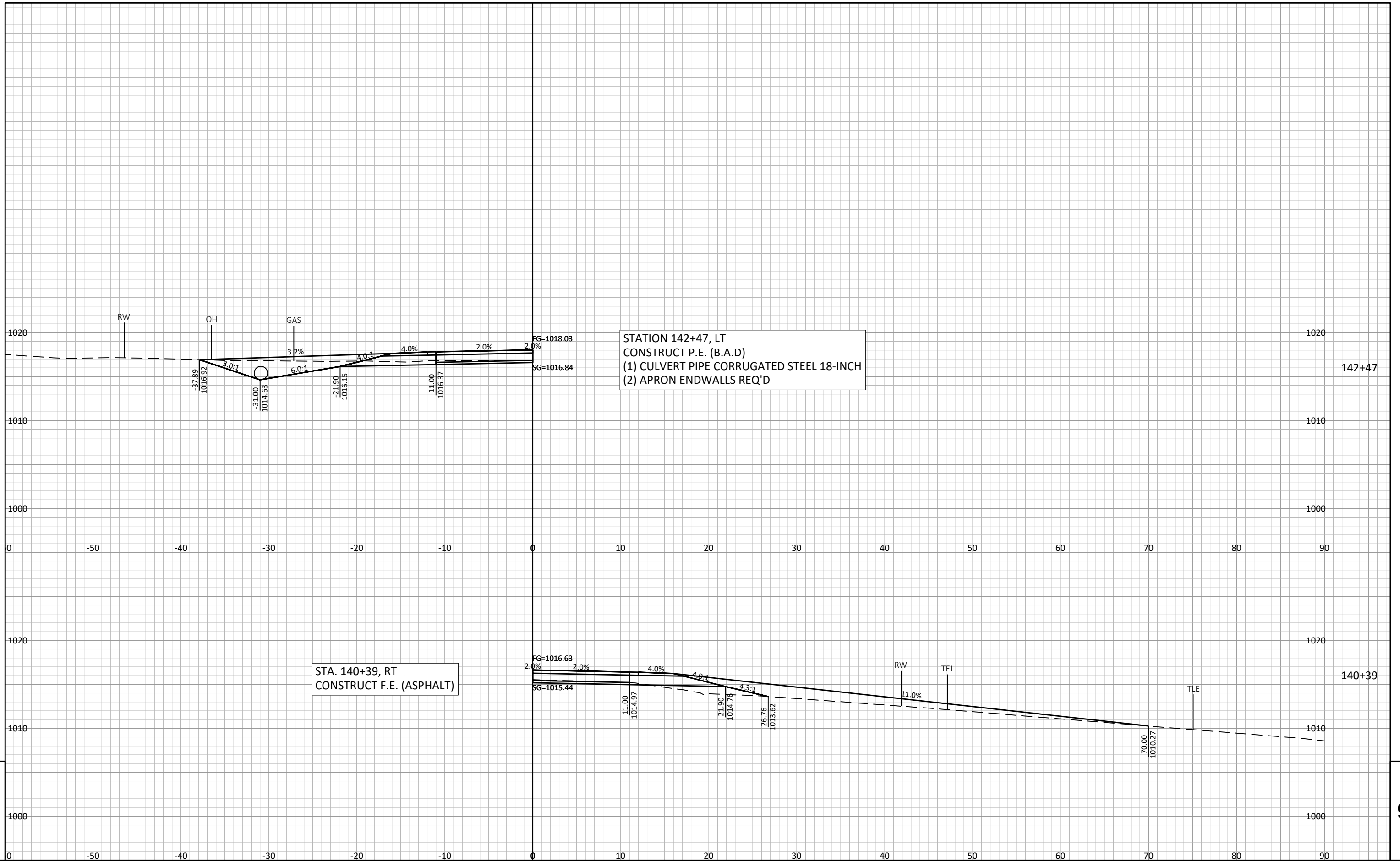
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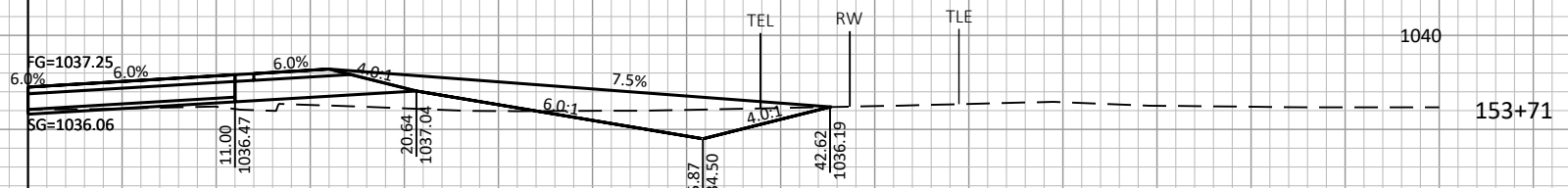
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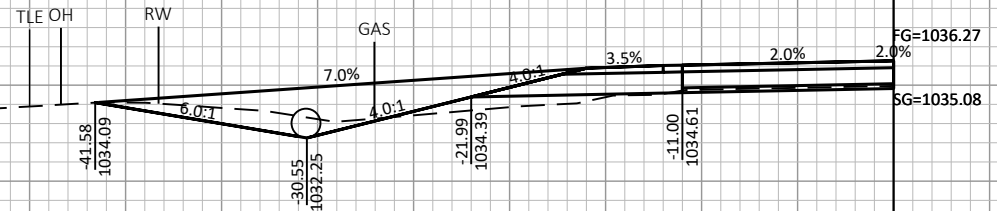
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STA. 153+71, RT
CONSTRUCT P.E. (ASPHALT)

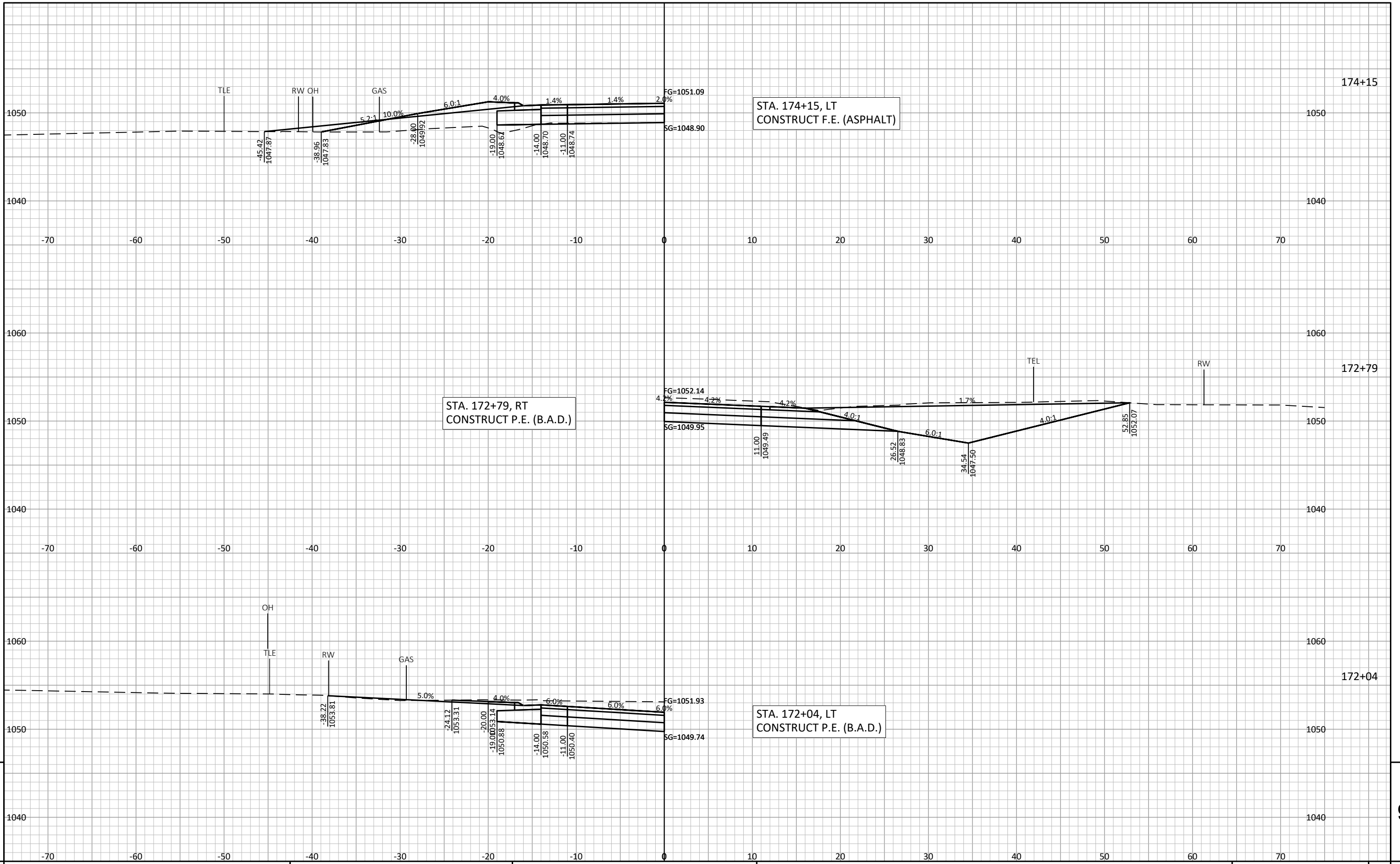


STATION 151+78, LT
CONSTRUCT F.E. (ASPHALT)
(1) CULVERT PIPE CORRUGATED STEEL 18-INCH
(2) APRON ENDWALLS REQ'D



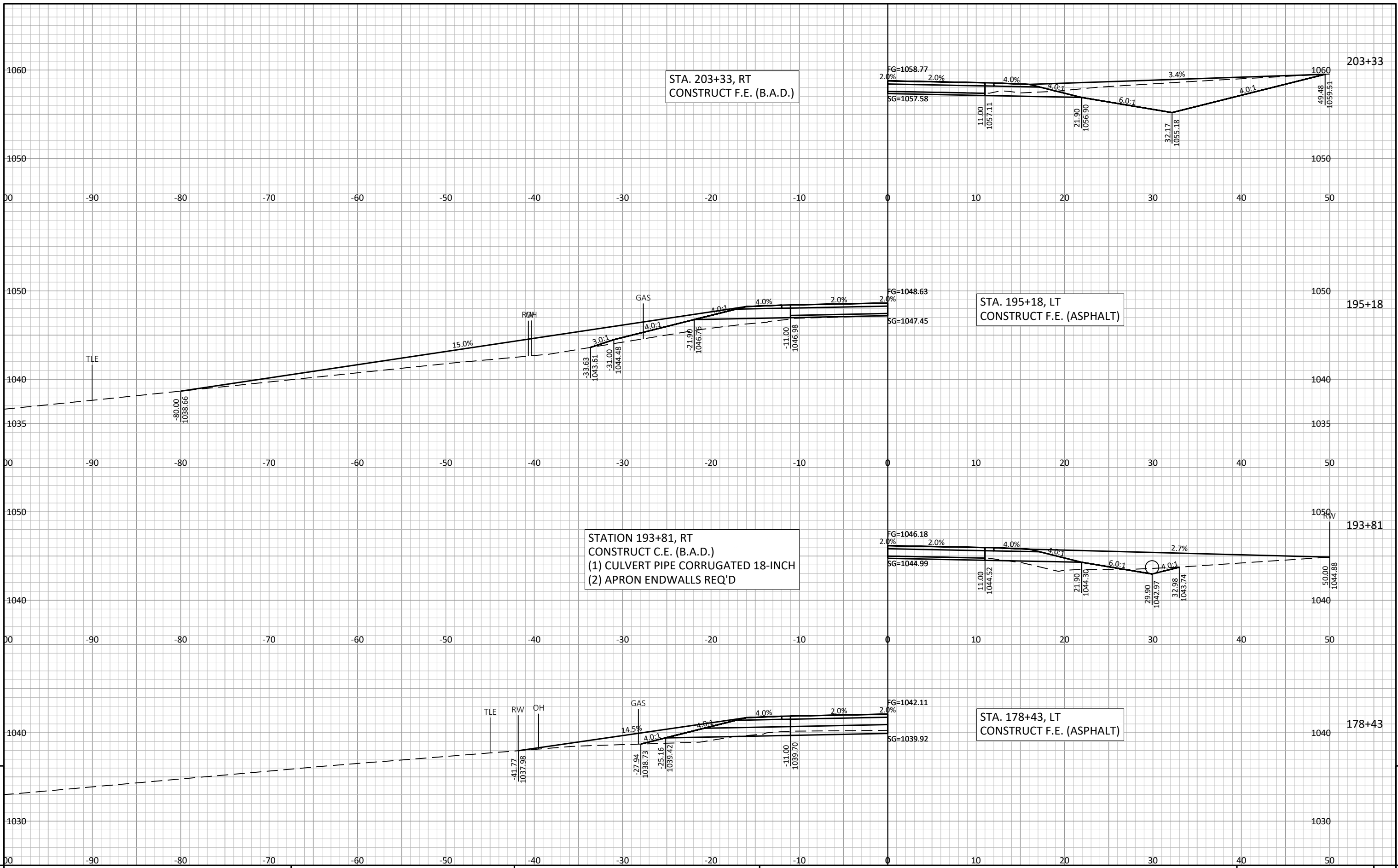
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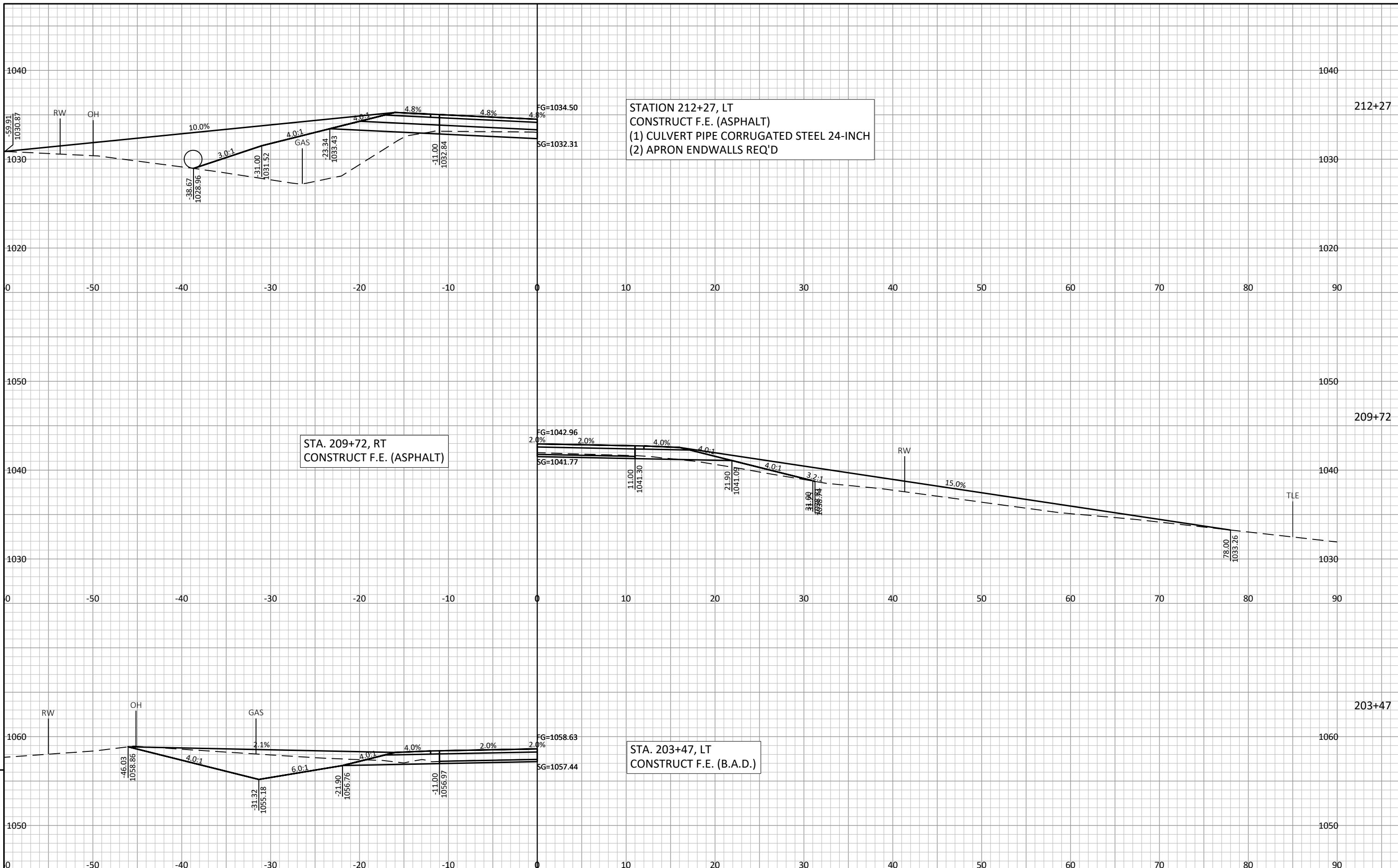
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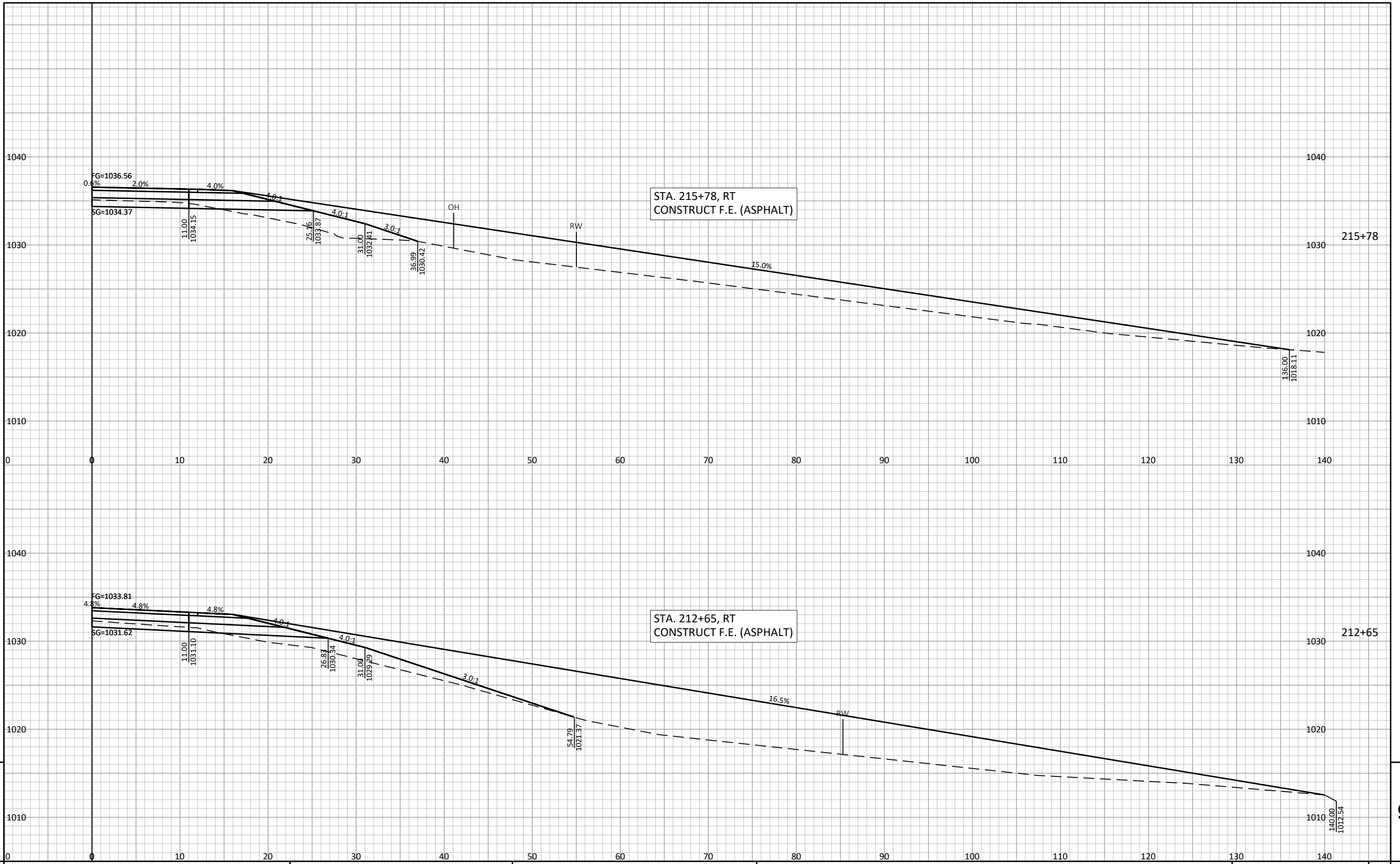
STA. 209+72, RT
CONSTRUCT F.E. (ASPHALT)

STATION 212+27, LT
CONSTRUCT F.E. (ASPHALT)
(1) CULVERT PIPE CORRUGATED STEEL 24-INCH
(2) APRON ENDWALLS REQ'D

STA. 203+47, LT
CONSTRUCT F.E. (B.A.D.)

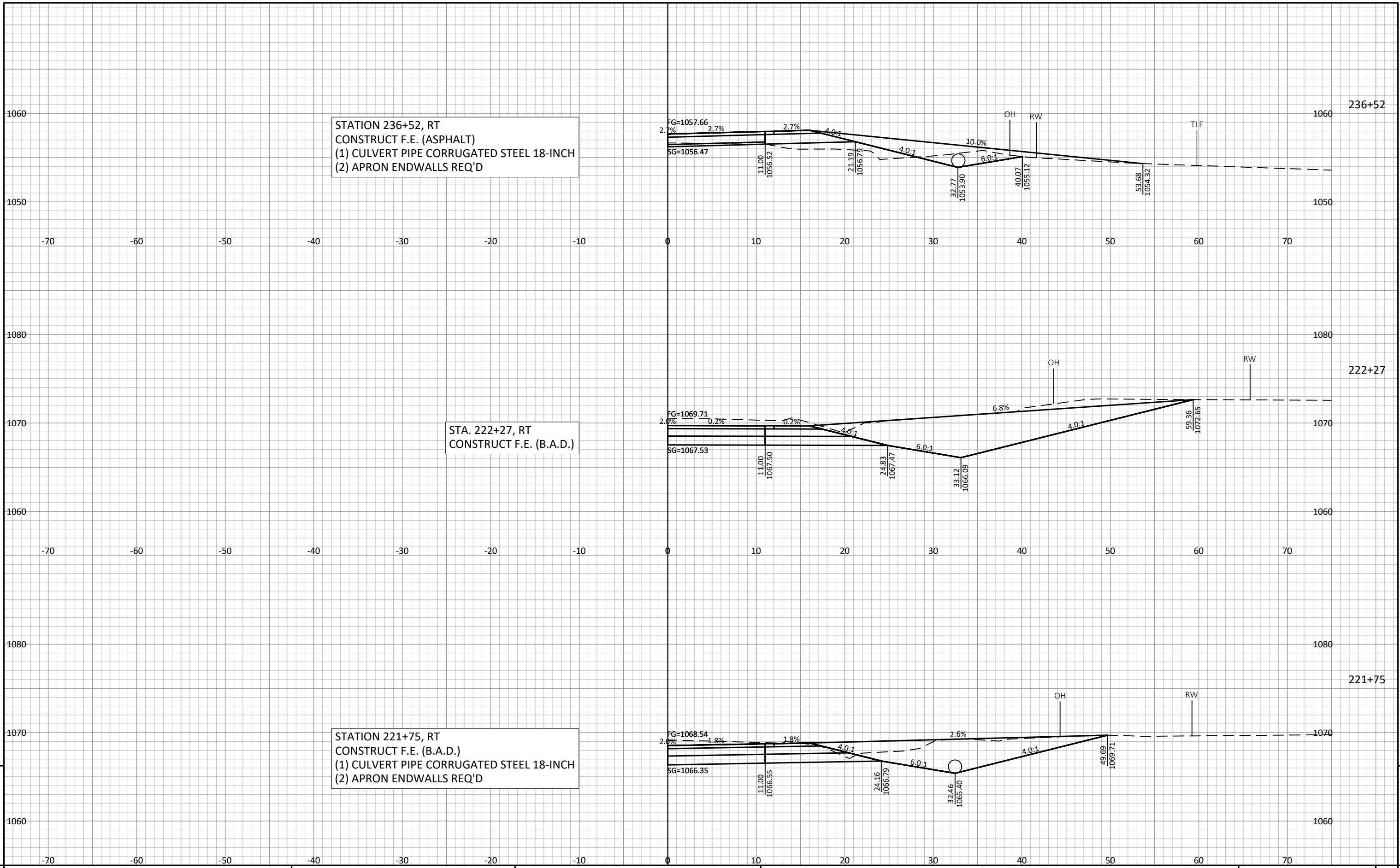
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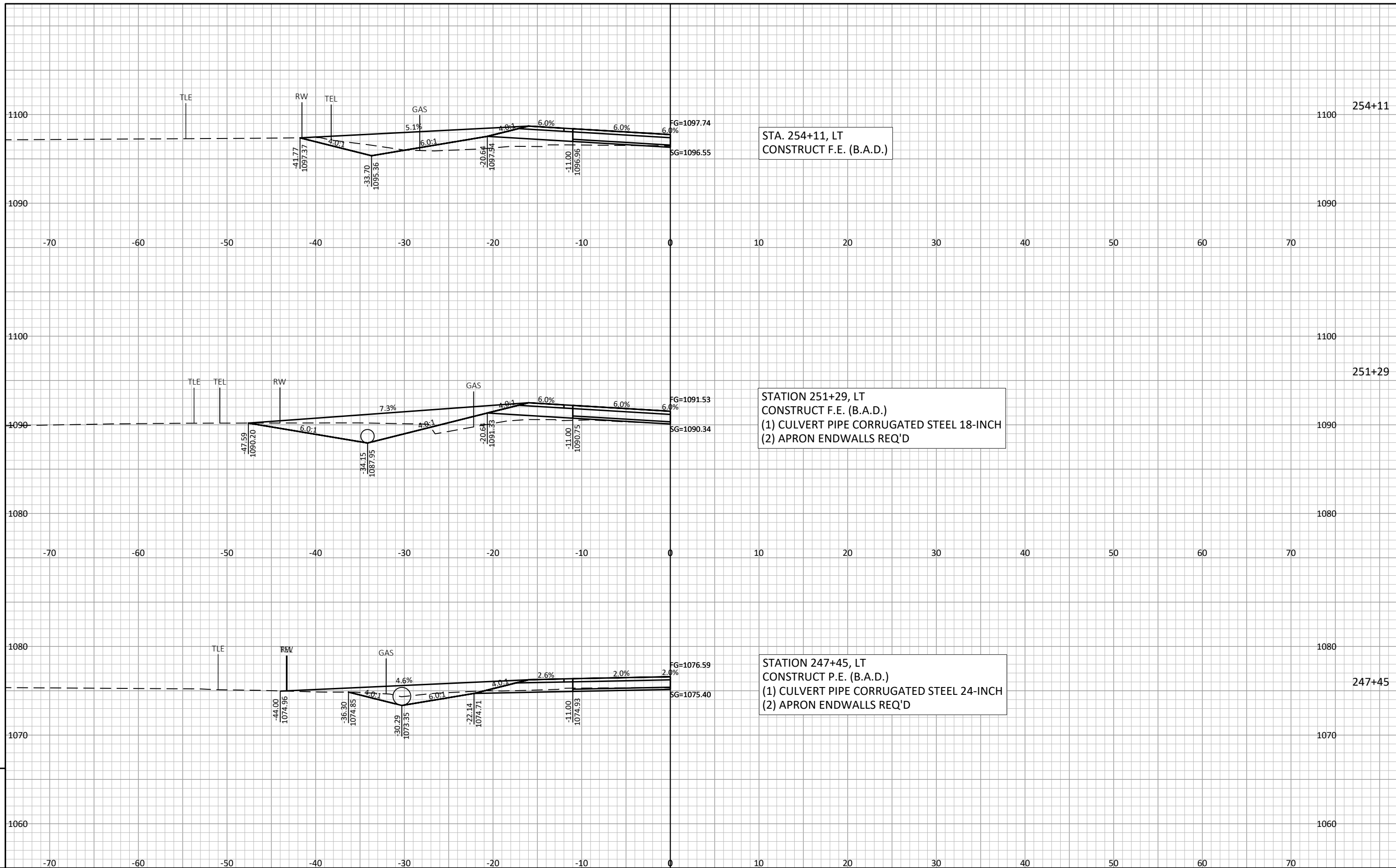
STATION 236+52, RT
 CONSTRUCT F.E. (ASPHALT)
 (1) CULVERT PIPE CORRUGATED STEEL 18-INCH
 (2) APRON ENDWALLS REQ'D

STA. 222+27, RT
 CONSTRUCT F.E. (B.A.D.)

STATION 221+75, RT
 CONSTRUCT F.E. (B.A.D.)
 (1) CULVERT PIPE CORRUGATED STEEL 18-INCH
 (2) APRON ENDWALLS REQ'D

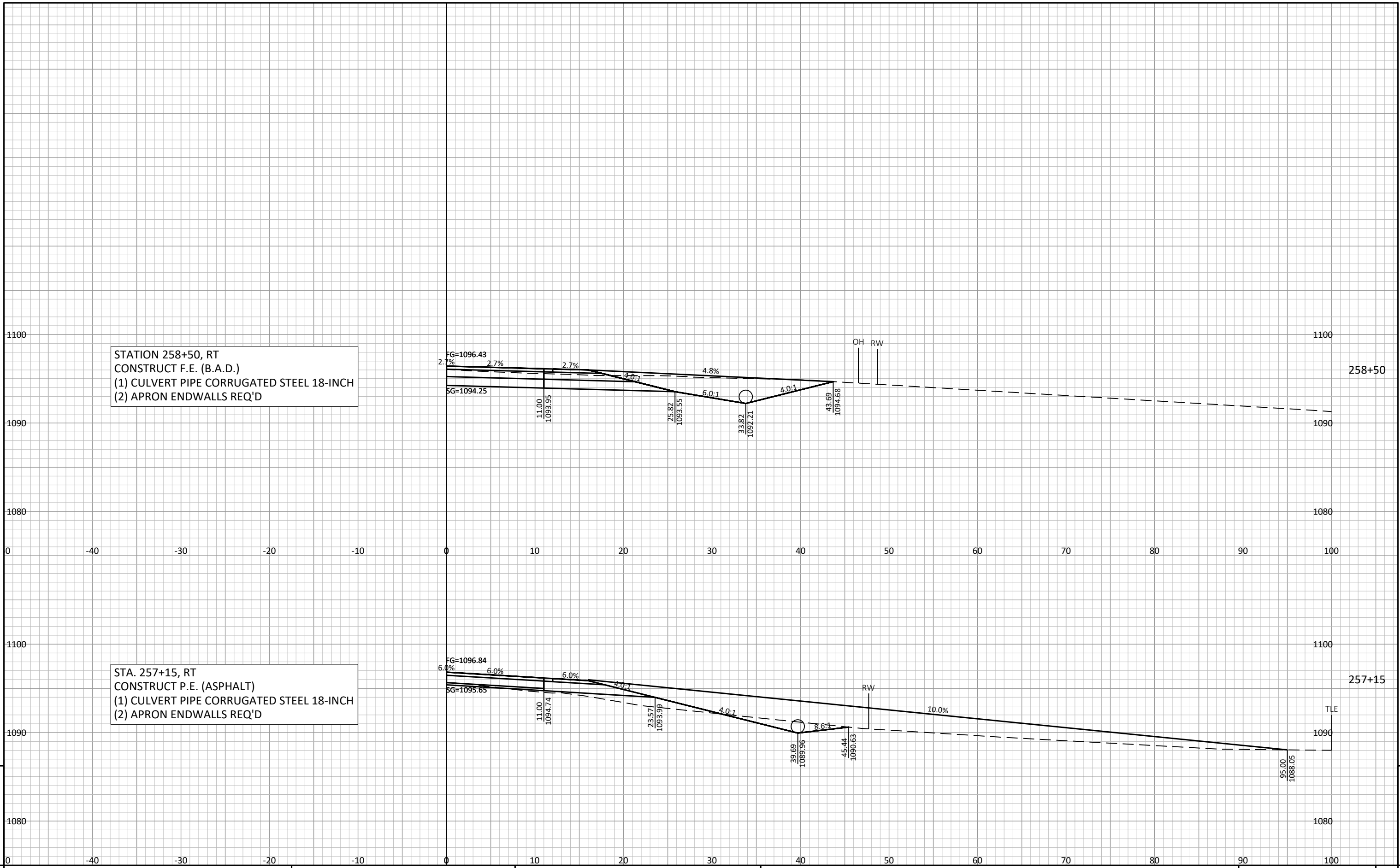
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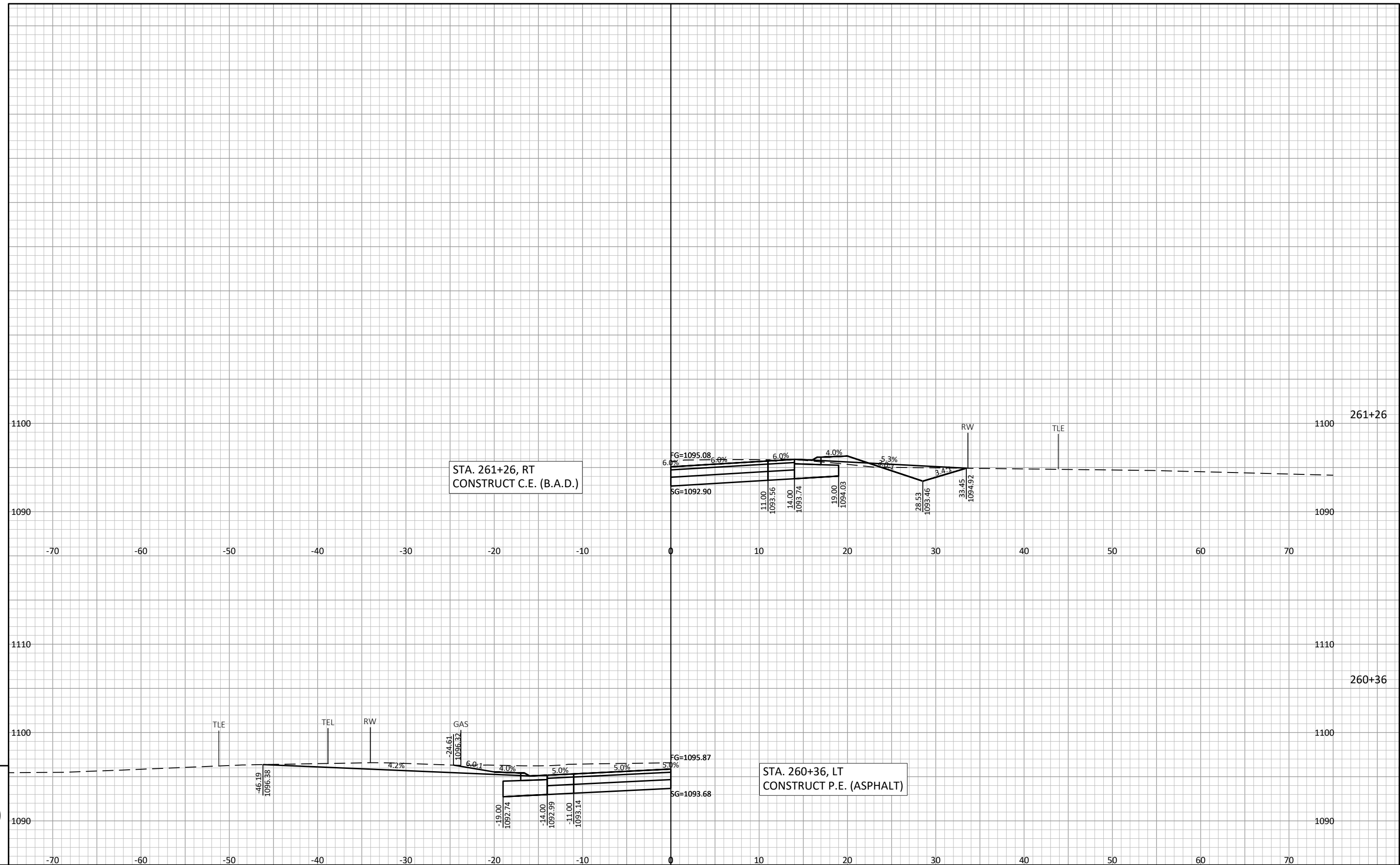
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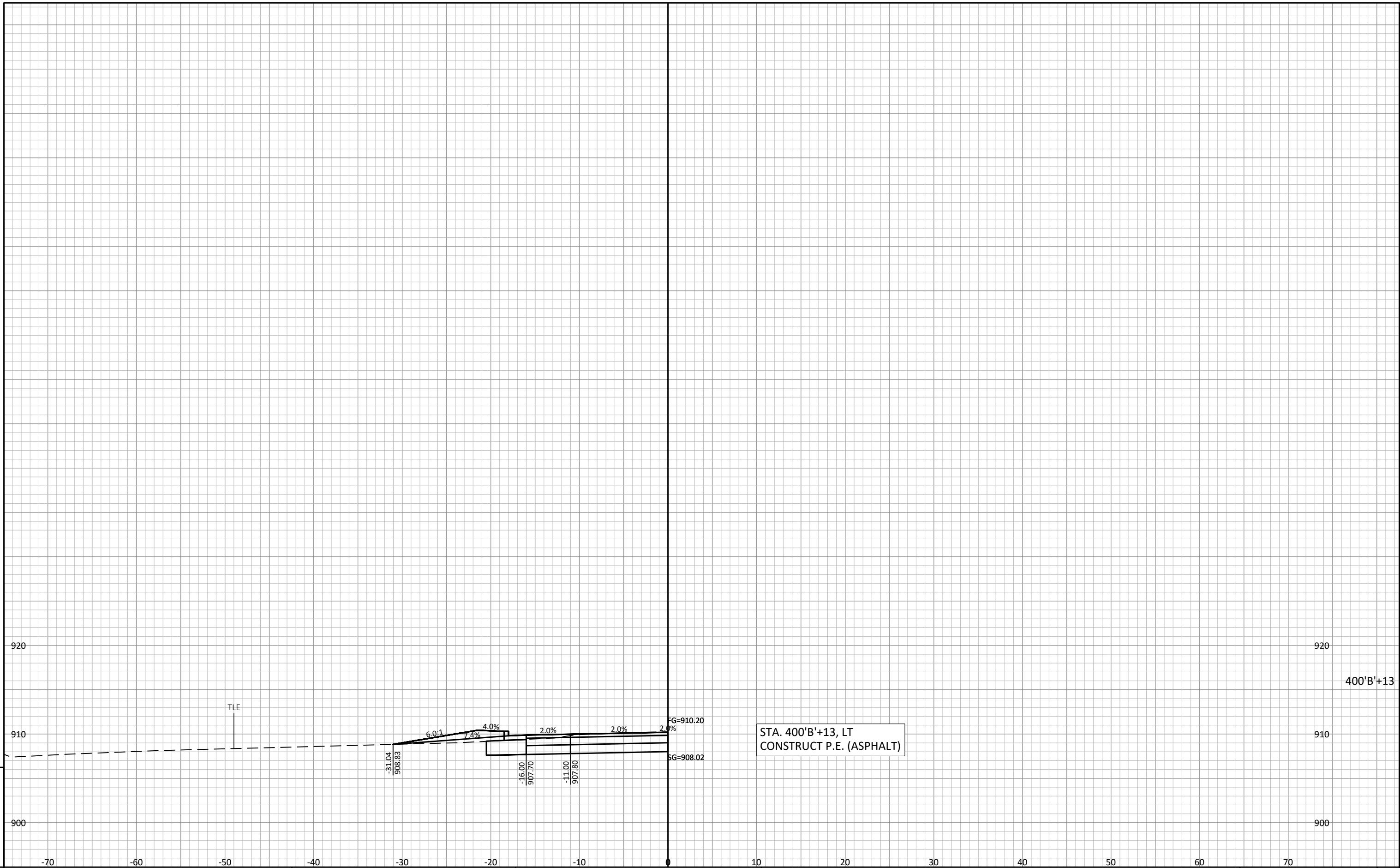
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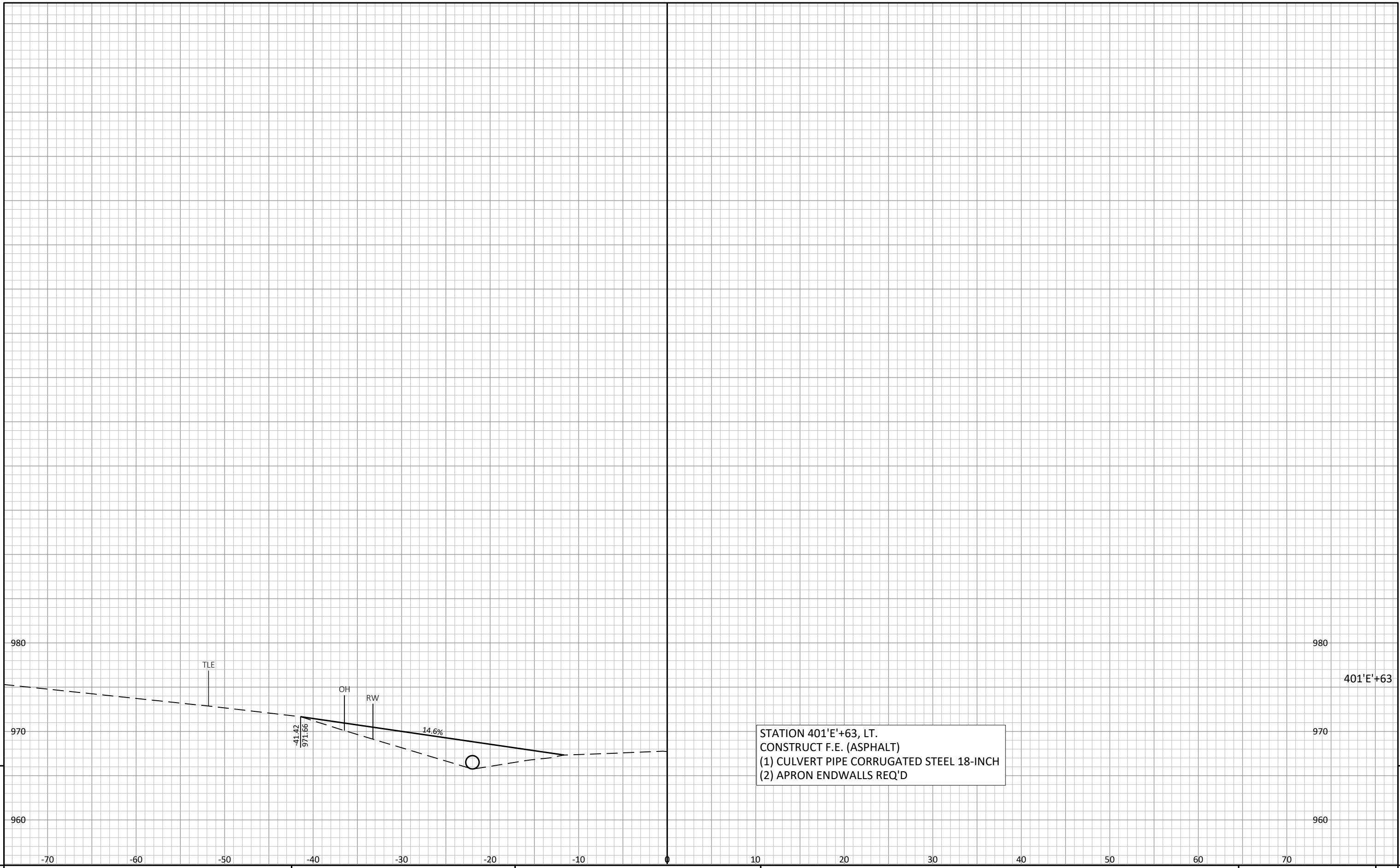


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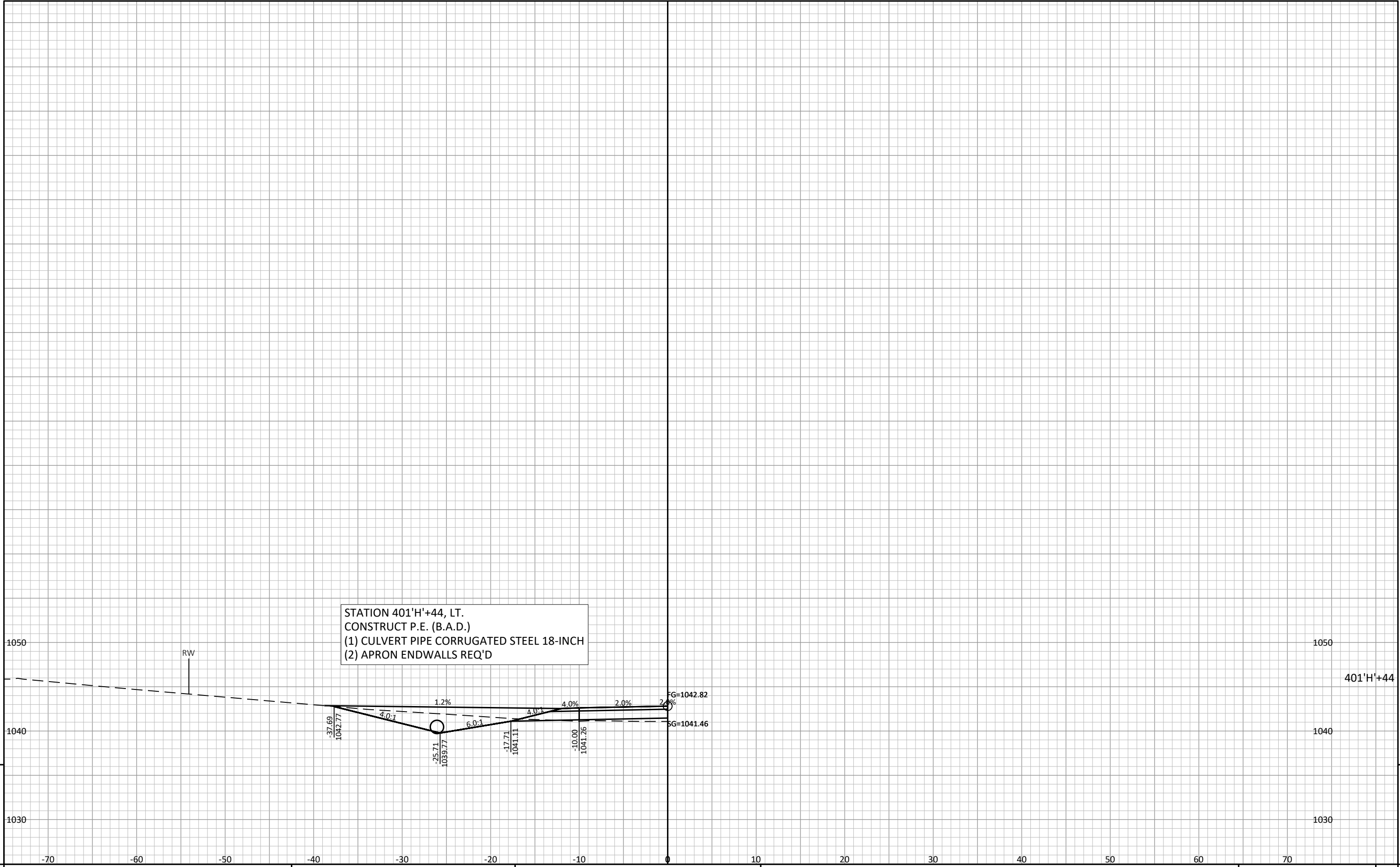




STATION 401'E'+63, LT.
 CONSTRUCT F.E. (ASPHALT)
 (1) CULVERT PIPE CORRUGATED STEEL 18-INCH
 (2) APRON ENDWALLS REQ'D

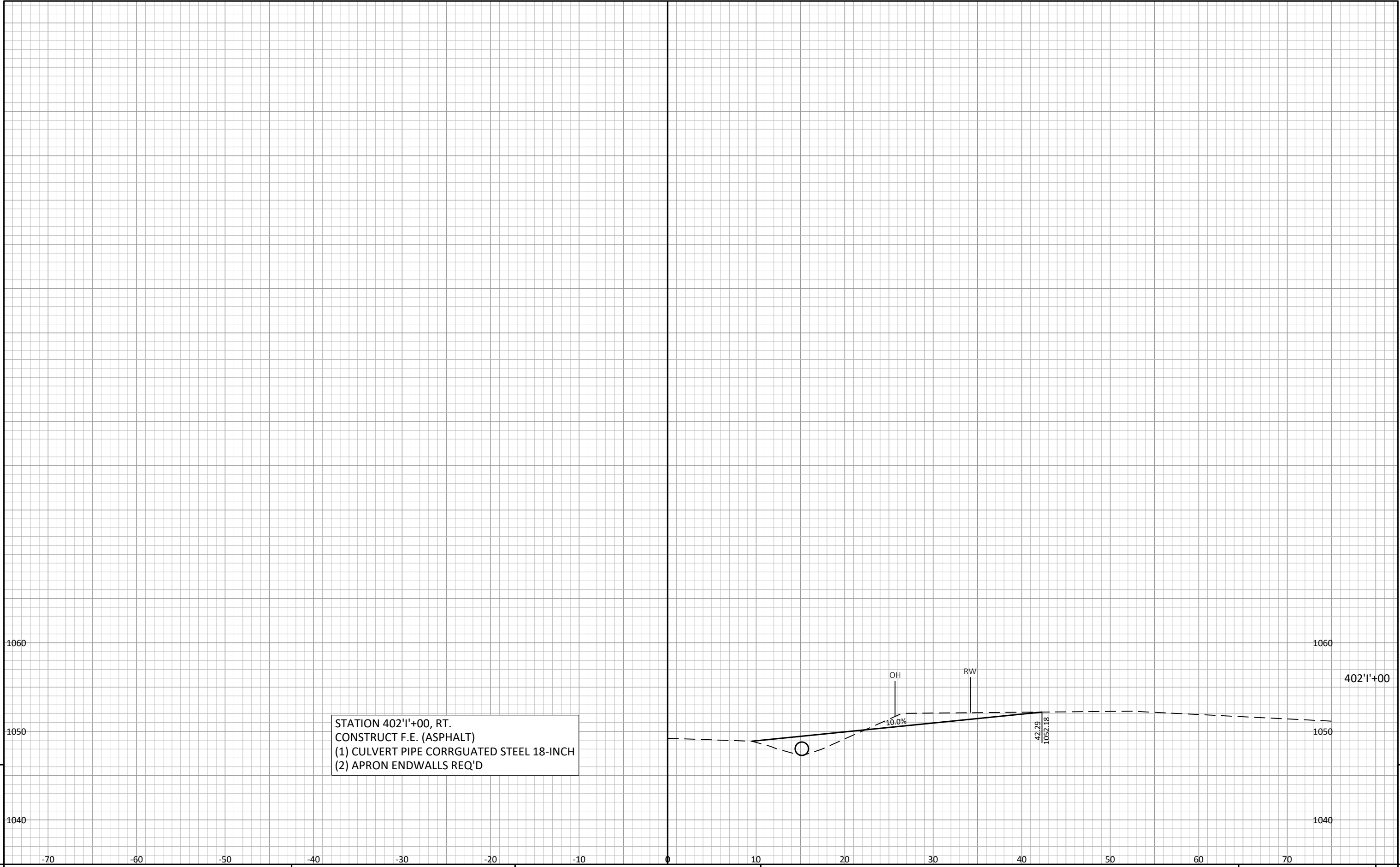
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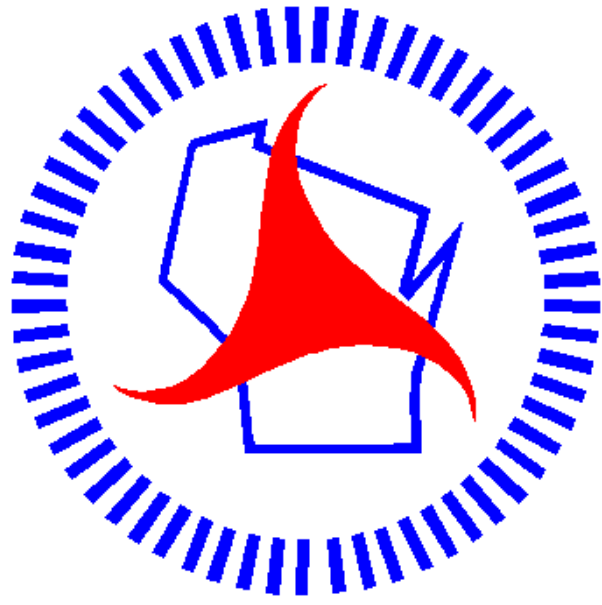
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Notes



Wisconsin Department of Transportation

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<http://www.dot.wisconsin.gov>