

MAD

NOVEMBER 2022

ORDER OF SHEETS

Section No.	1	Title
Section No.	2	Typical Sections and Details
Section No.	3	Estimate of Quantities
Section No.	3	Miscellaneous Quantities
Section No.	4	Right of Way Plat
Section No.	5	Plan and Profile
Section No.	6	Standard Detail Drawings
Section No.	7	Sign Plates
Section No.	8	Structure Plans
Section No.	8	Computer Earthwork Data
Section No.	9	Gross Sections

TOTAL SHEETS = 106



DESIGN DESIGNATION

A.A.D.T.	2022	=	350
A.A.D.T.	2041	=	385
D.H.V.		=	39
D.D.		=	50/50
T.		=	18%
DESIGN SPEED		=	55 MPH
ESALS		=	91,000

CONVENTIONAL SYMBOLS

PLAN	PROFILE
CORPORATE LIMITS	GRADE LINE
PROPERTY LINE	ORIGINAL GROUND
LOT LINE	MARSH OR ROCK PROFILE (To be noted as such)
LIMITED HIGHWAY EASEMENT	SPECIAL DITCH
EXISTING RIGHT OF WAY	GRADE ELEVATION
PROPOSED OR NEW R/W LINE	CULVERT (Profile View)
SLOPE INTERCEPT	UTILITIES
REFERENCE LINE	ELECTRIC
EXISTING CULVERT	FIBER OPTIC
PROPOSED CULVERT (Box or Pipe)	GAS
COMBUSTIBLE FLUIDS	SANITARY SEWER
MARSH AREA	STORM SEWER
WOODED OR SHRUB AREA	TELEPHONE
	WATER
	UTILITY PEDESTAL
	POWER POLE
	TELEPHONE POLE

# STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

## PLAN OF PROPOSED IMPROVEMENT

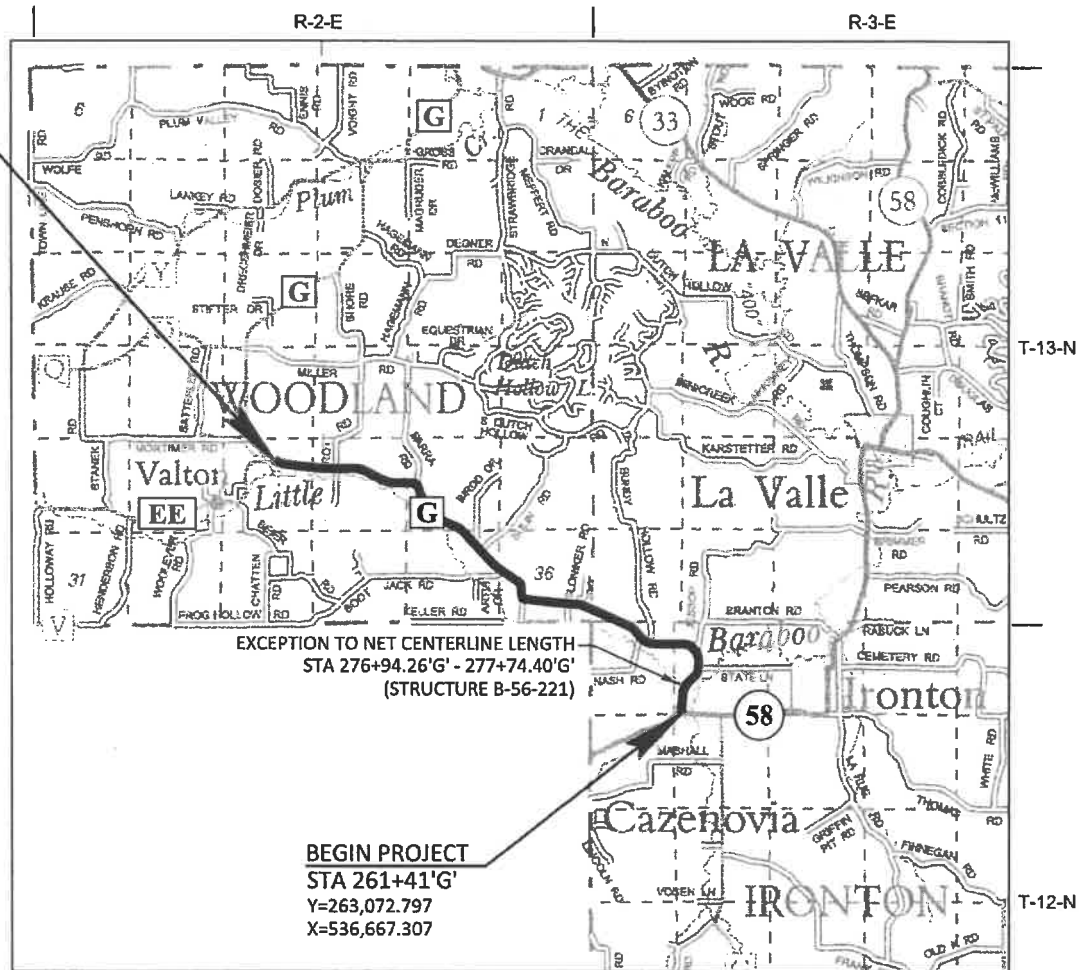
### IRONTON - WOODLAND (CTH G)

STH 58 to CTH EE

CTH G

SAUK COUNTY

STATE PROJECT NUMBER  
**5908-00-74**



LAYOUT  
SCALE 0 2 MI

TOTAL NET LENGTH OF CENTERLINE = 6.047 MI

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS), SAUK COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES.

ELEVATIONS ARE REFERENCED TO NAVD 88 (2012). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 12A.

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
5908-00-74	WISC 2023065	1

ACCEPTED FOR

COUNTY of SAUK

7/20/2022 Highway Commissioner  
(Date) (Signature & Title of Official)

ORIGINAL PLANS PREPARED BY

**SEH** Short Elliott Hendrickson Inc.  
329 Jay Street, Suite 301  
La Crosse, WI 54601-4007  
608.782.3161 main | 888.808.8166 fax  
Building a Better World for All of Us www.sehinc.com



7/20/2022  
(Date) (Signature)

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

PREPARED BY	SEH
Surveyor	SEH
Designer	SEH
Project Manager	DELLA KOENIG P.E.
Regional Examiner	REGION
Regional Supervisor	KYLE HEMP P.E.

APPROVED FOR THE DEPARTMENT  
DATE: 7/26/2022  
(Signature)

E

PROJECT ID:

5908-00-74

17

COUNTY:

SAUK

**STANDARD ABBREVIATIONS**

ABUT	ABUTMENT	ID	INSIDE DIAMETER
AC	ACRE	INV	INVERT
AGG	AGGREGATE	IP	IRON PIPE ON PIN
AECPRC	APRON ENDWALL FOR CULVERT PIPE REINFORCED CONCRETE	LHF	LEFT-HAND FORWARD
AECPCS	APRON ENDWALL FOR CULVERT PIPE CORRUGATED STEEL	L	LENGTH OF CURVE
ASPH	ASPHALTIC	LF	LINEAR FOOT
AVG	AVERAGE	LC	LONG CHORD OF CURVE
ADT	AVERAGE DAILY TRAFFIC	LS	LUMP SUM
BF	BACK FACE	MH	MANHOLE
BM	BENCH MARK	MOR	MID POINT OF RADIUS
BR	BRIDGE	NC	NORMAL CROWN
CE	COMMERCIAL ENTRANCE	NO	NUMBER
C/L	CENTER LINE	OBLIT	OBLITERATE
Δ	CENTRAL ANGLE OR DELTA	PAVT	PAVEMENT
COB	CENTER OF BARRIER	PE	PRIVATE ENTRANCE
CONC	CONCRETE	PVRC	POINT OF VERTICAL REVERSE CURVE
CPRC	CULVERT PIPE REINFORCED CONCRETE	QOR	QUARTER POINT OF RADIUS
CPRCHE	CULVERT PIPE REINFORCED CONCRETE HORIZONTAL ELLIPTICAL	R	RADIUS
CR	CREEK	REQ'D	REQUIRED
CY	CUBIC YARD	RES	RESIDENCE OR RESIDENTIAL
C&G	CURB AND GUTTER	RHF	RIGHT-HAND FORWARD
D	DEGREE OF CURVE	R/W	RIGHT-OF-WAY
DHV	DESIGN HOUR VOLUME	R	RIVER
DISCH	DISCHARGE	RDWY	ROADWAY
DG	DITCH GRADE	R/L	REFERENCE LINE
DWY	DRIVEWAY	SALV	SALVAGED
X	EAST GRID COORDINATE	SAN	SANITARY SEWER
EAT	STEEL PLATE BEAM GUARD ENERGY ABSORBING TERMINAL	SF	SQUARE FEET
EOR	END POINT OF RADIUS	SY	SQUARE YARD
EL	ELEVATION	SDD	STANDARD DETAIL DRAWINGS
ENT	ENTRANCE	STA	STATION
ESALS	EQUIVALENT SINGLE AXLE LOADS	SS	STORM SEWER
EXC	EXCAVATION	SSPRC	STORM SEWER PIPE REINFORCED CONCRETE
EBS	EXCAVATION BELOW SUBGRADE	SE	SUPERELEVATION RATE
EXIST	EXISTING	TC	TOP OF CURB
FC	FACE OF CURB	T OR TN	TOWN
FF	FACE TO FACE	T	TRUCKS (PERCENT OF)
FERT	FERTILIZE	TYP	TYPICAL
FE	FIELD ENTRANCE	VAR	VARIABLE
FL	FLOW LINE	VC	VERTICAL CURVE
FO	FIBER OPTIC	Y	NORTH GRID COORDINATE
CWT	HUNDREDWEIGHT	YD	YARD
HYD	HYDRANT		

**DNR AREA LIAISON:**

WI DEPT OF NATURAL RESOURCES  
DNR SERVICE CENTER  
3550 MORMON COULEE RD  
LA CROSSE, WI 54601  
TELEPHONE: 608.785.9115  
ATTENTION: KAREN KALVELAGE  
EMAIL: KAREN.KALVELAGE@WISCONSIN.GOV

**WISDOT CONTACT:**

WISCONSIN DEPT OF TRANSPORTATION  
SOUTHWEST REGION  
2101 WRIGHT STREET  
MADISON, WI 53704  
TELEPHONE: 608.246.7963  
ATTENTION: DELLA KOENIG  
EMAIL: DELLA.KOENIG@DOT.WI.GOV

**DESIGN CONTACT:**

SEH INC.  
329 JAY STREET, SUITE 301  
LA CROSSE, WI 54601  
TELEPHONE: 608.498.4804  
ATTENTION: ALEIGHA BURG  
EMAIL: ABURG@SEHINC.COM

**SAUK COUNTY:**

SAUK COUNTY HIGHWAY COMMISSIONER  
620 LINN STREET  
PO BOX 26  
WEST BARABOO, WI 53913  
TELEPHONE: 608.355.4855  
ATTENTION: PAT GAVINSKI  
EMAIL: PATRICK.GAVINSKI@SAUKCOUNTYWI.GOV

**UTILITY CONTACT LIST:**

ALLIANT ENERGY - ELECTRICITY  
520 COMMERCE AVE.  
BARABOO, WI 53913  
ATTENTION: MIKE LONG  
TELEPHONE: 608.356.0608  
EMAIL: MIKELONG@ALLIANTENERGY.COM

VERNON ELECTRIC - ELECTRICITY  
110 SAUGSTAD ROAD  
WESTBY, WI 54667  
ATTENTION: COLE CARY  
TELEPHONE: 608.634.7472  
EMAIL: CCARY@VERNONELECTRIC.COM

LA VALLE TELEPHONE - COMMUNICATION LINE  
51421 STATE HIGHWAY 33  
PO BOX 28  
LA VALLE, WI 53941  
ATTENTION: JOSH LIEN  
TELEPHONE: 608.985.7201  
EMAIL: JOSH@RGTC.COOP

VILLAGE OF CAZENOVIA - SANITARY  
115 CUNNINGHAM RIDGE ROAD  
CAZENOVIA, WI 53924  
ATTENTION: DENNIS ADELMAN  
TELEPHONE: 608.963.8221  
EMAIL: ADELMAN@MWT.NET

LUMEN - COMMUNICATION LINE  
130 FOURTH STREET  
BARABOO, WI 53913  
ATTENTION: BRIAN STELPLUGH  
TELEPHONE: 608.615.4138  
EMAIL: BRIAN.STELPLUGH@LUMEN.COM

**GENERAL NOTES:**

- NO TREES OR SHRUBS SHALL BE REMOVED WITHOUT APPROVAL OF THE ENGINEER.
- THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.
- THE CONTRACTOR SHALL NOTIFY DIGGERS HOTLINE AND AFFECTED UTILITIES PRIOR TO THE START OF WORK. ANY LOCAL MUNICIPAL UTILITY WHICH IS NOT A MEMBER OF THE DIGGERS HOTLINE MUST BE CONTACTED SEPARATELY.
- WETLANDS, WATERWAYS, AND OTHER ENVIRONMENTALLY SENSITIVE AREAS SHALL BE PROTECTED AT ALL TIMES. DO NOT STORE EQUIPMENT OR MATERIALS NEAR THESE SITES UNLESS APPROVED BY THE ENGINEER.
- TRAFFIC CONTROL DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.
- ASPHALTIC AND CONCRETE SURFACES SHALL BE SAWCUT AT THE MATCH LINE AS SHOWN ON THE PLAN OR AS DIRECTED BY THE ENGINEER.
- CONTRACTOR WILL BE RESPONSIBLE FOR RESHAPING AND SEEDING ANY PREVIOUSLY GRASSED AREAS WHICH ARE DISTURBED BY HIS OPERATION OUTSIDE OF THE NORMAL CONSTRUCTION LIMITS.
- FERTILIZER SHALL NOT BE USED NEAR NAVIGABLE WATERWAYS OR WETLANDS.
- APPLY TACK COAT AT A RATE OF 0.07 GAL/SY TO MILLED SURFACE AND 0.05 GA/SY BETWEEN LAYERS OF HMA PAVEMENT.
- HMA PAVEMENT WEIGHT CALCULATIONS ARE BASED ON 112 LB/SY/IN.
- THE CONTRACTOR'S PAVING OPERATIONS SHALL BE CONSISTENT WITH THE PLAN AND TYPICAL SECTIONS AND CONSTRUCTED TO PREVENT HMA LONGITUDINAL JOINTS FROM BEING LOCATED WITHIN A DRIVING, TURNING, PASSING OR PARKING LANE.

**RUNOFF COEFFICIENT TABLE**

	HYDROLOGIC SOIL GROUP											
	A			B			C			D		
	SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)		
LAND USE:	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER
ROW CROPS	.08 .22	.16 .30	.22 .38	.12 .26	.20 .34	.27 .44	.15 .30	.24 .37	.33 .50	.19 .34	.28 .41	.38 .56
MEDIAN STRIP-TURF	.19 .24	.20 .26	.24 .30	.19 .25	.22 .28	.26 .33	.20 .26	.23 .30	.30 .37	.20 .27	.25 .32	.30 .40
SIDE SLOPE-TURF			.25 .32			.27 .34			.28 .36			.30 .38
PAVEMENT:												
ASPHALT	.70 - .95											
CONCRETE	.80 - .95											
BRICK	.70 - .80											
DRIVES, WALKS	.75 - .85											
ROOFS	.75 - .95											
GRAVEL ROADS, SHOULDERS	.40 - .60											

TOTAL PROJECT AREA = 49.3 ACRES  
TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = 0.0 ACRES



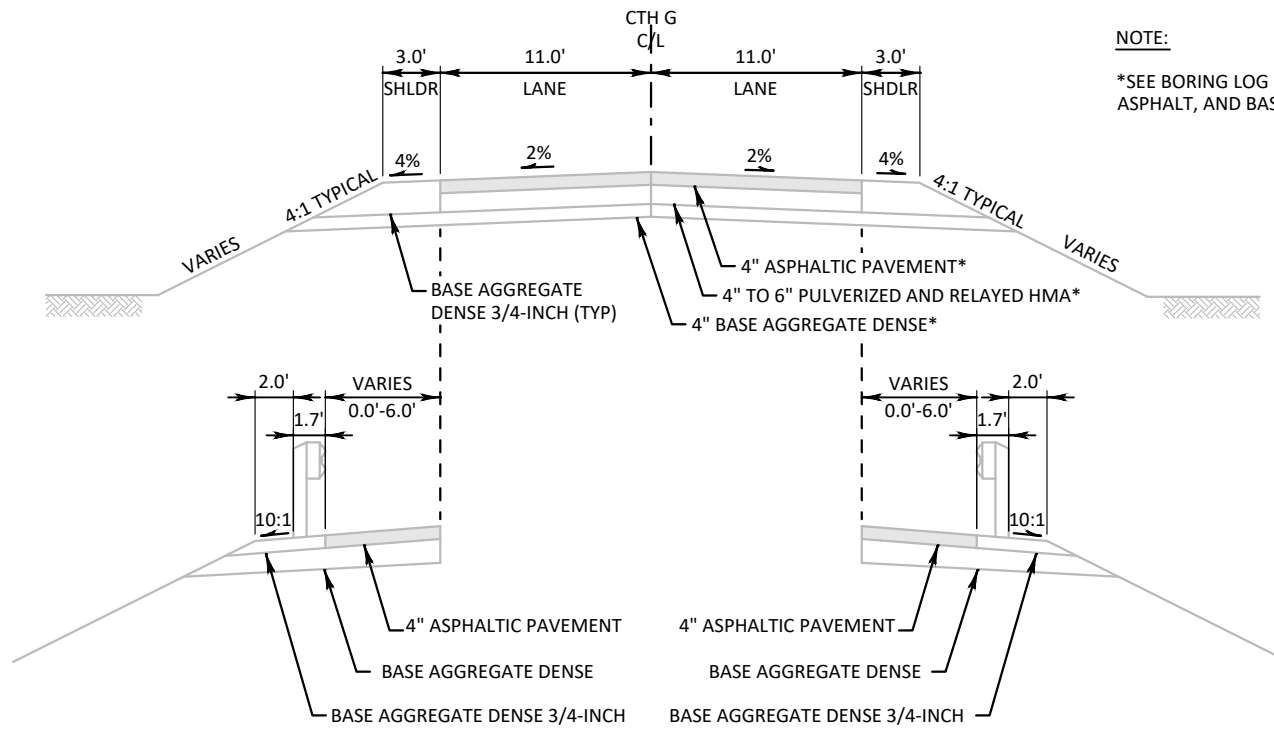
Dial 811 or (800)242-8511

www.DiggersHotline.com

**BORING LOG:**

BORING NO.	STATION	ASPHALTIC PAVEMENT DEPTH (IN)	PULVERIZED HMA (IN)	BASE DEPTH (IN)	SUBBASE MATERIAL
B-1	265+00	4	6	6	Silt Sand
B-2	320+85	6	2	12	Silty Sand
B-3	374+12	4	8	4	Green Clay / Silt
B-4	428+38	7	2	4	Brown Clay
B-5	489+21	4	6	2	Sand
B-6	543+70	4	6	3	Sand / Gravel



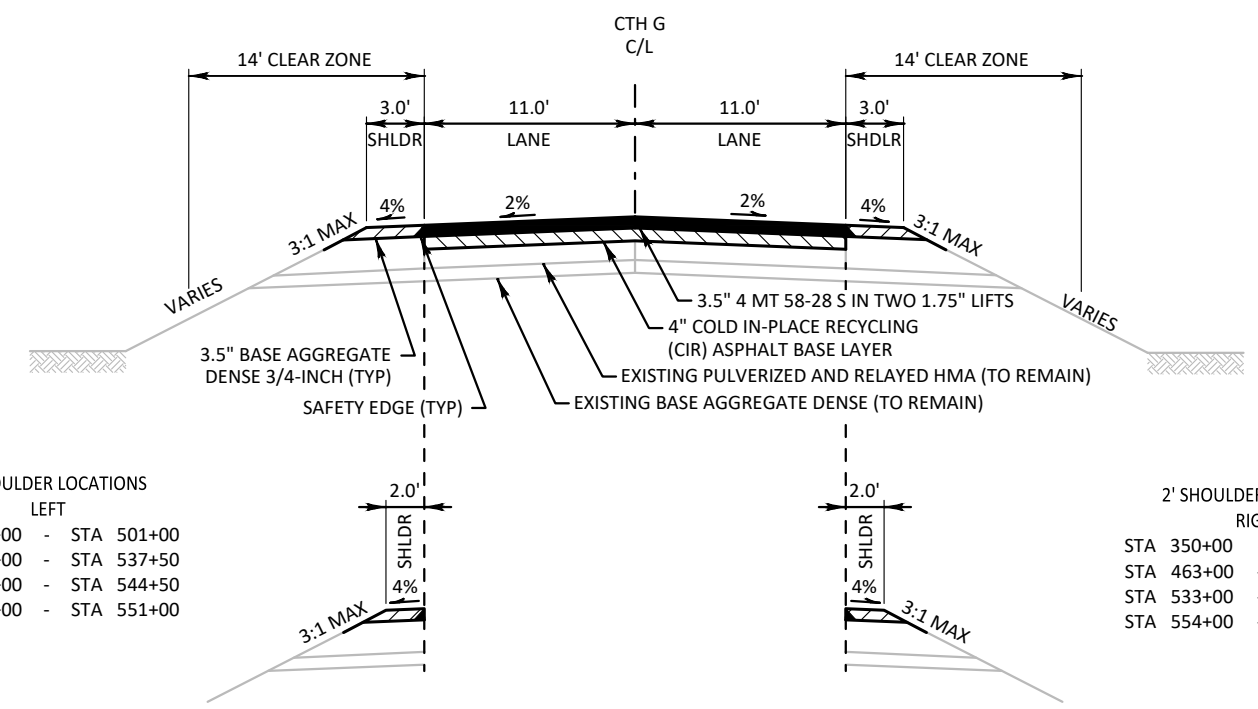


GUARD RAIL LOCATIONS  
LEFT  
STA 269+38 - STA 269+64  
STA 275+26 - STA 278+44  
STA 551+07 - STA 551+34

GUARD RAIL LOCATIONS  
RIGHT  
STA 269+38 - STA 269+64  
STA 276+76 - STA 278+44  
STA 551+07 - STA 551+34

TYPICAL EXISTING SECTION

CTH G  
STA 261+41 - STA 581+50



2' SHOULDER LOCATIONS  
LEFT  
STA 493+00 - STA 501+00  
STA 532+00 - STA 537+50  
STA 542+00 - STA 544+50  
STA 546+00 - STA 551+00

2' SHOULDER LOCATIONS  
RIGHT  
STA 350+00 - STA 359+00  
STA 463+00 - STA 502+00  
STA 533+00 - STA 544+50  
STA 554+00 - STA 558+00

TYPICAL FINISHED SECTION

CTH G  
STA 261+41 - STA 581+50

PROJECT NO:

SHEET

5908-00-74

HWY: CTH G

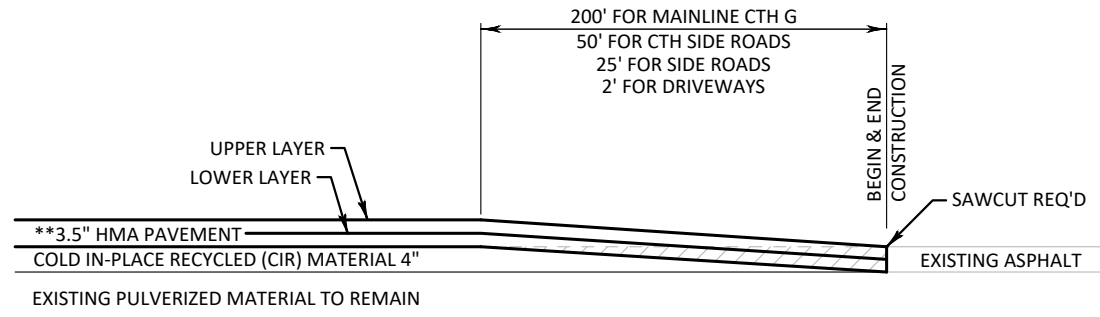
COUNTY: SAUK

TYPICAL SECTIONS

LAYOUT NAME:

E





REMOVING ASPHALTIC SURFACE BUTT JOINTS

**MAINLINE**  
 STA 261+41 - STA 263+41  
 STA 274+94 - STA 276+94 (B-56-221)  
 STA 277+74 - STA 279+74 (B-56-221)

**\*\*SIDE ROADS**

JESSOP RD	CHURCH RD	FARRA RD (EAST)
BAUER RD	QUAKER VALLEY RD	ROTT RD (NORTH)
BUNDY HOLLOW RD	BOOT JACK RD	CTH EE
NASH RD	BULIN RD	
SLONIKER RD	BIRDD RD	
NORWEGIAN	FARRA RD (SOUTH)	

\*\*3 MT 58-28 S, 3-INCH, FOR SIDEROADS

REMOVING ASPHALTIC SURFACE BUTT JOINTS

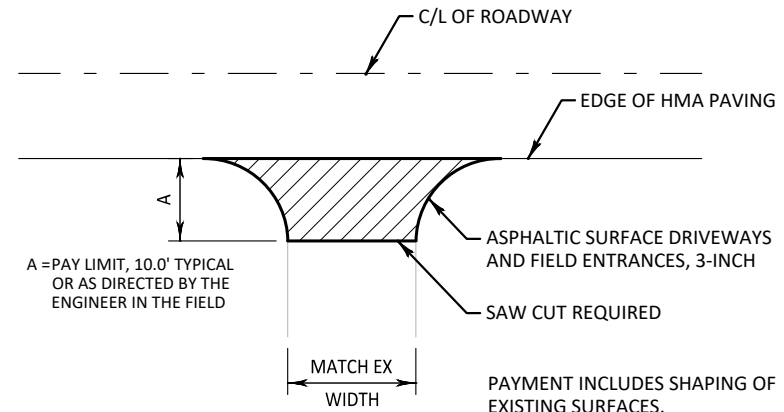
200' FOR MAINLINE CTH G  
 50' FOR CTH SIDE ROADS  
 25' FOR SIDE ROADS  
 2' FOR DRIVEWAYS

BEGIN & END  
 CONSTRUCTION

SAWCUT REQ'D

EXISTING ASPHALT

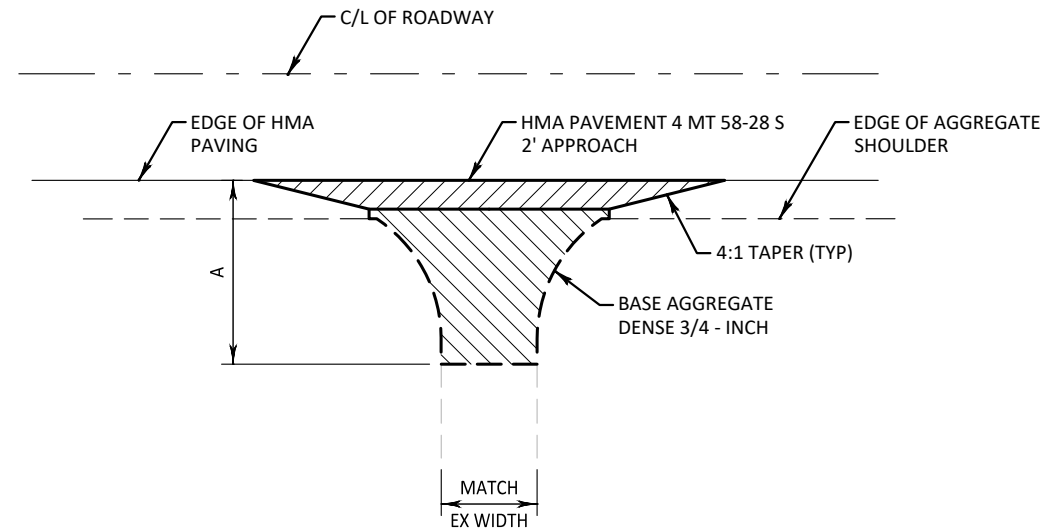
EXISTING PULVERIZED MATERIAL TO REMAIN



PAYMENT INCLUDES SHAPING OF EXISTING SURFACES.

ANY ADDITIONAL BASE AGGREGATE DENSE REQUIRED SHALL BE PAID UNDER ITEM - "BASE AGGREGATE DENSE 3/4-INCH".

ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTERANCES  
 (PE, CE, OR FE)



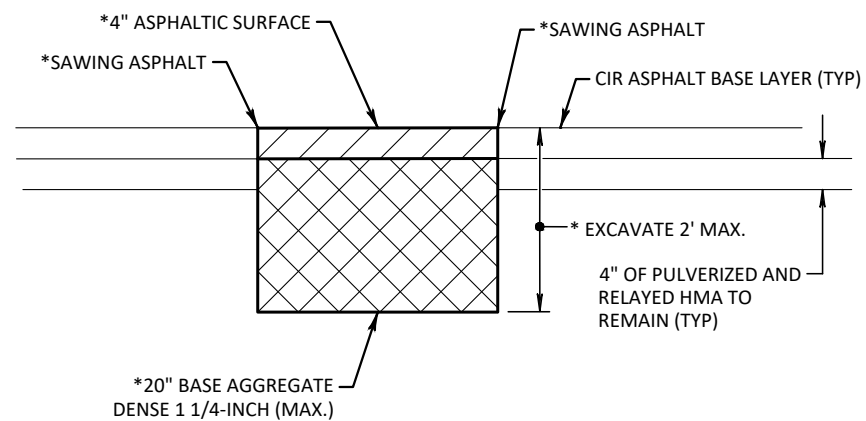
NOTE, 2' HMA APPROACH NOT SHOWN ON PLAN SHEETS

RURAL DRIVEWAY OR FIELD ENTRANCE  
 AGGREGATE (PE, CE OR FE)

SITE NO	PHASE 1	PHASE 2
LOCATION	(2 SEC)	(2 SEC)
SITE 1	CTH G	STARTING
STA 262+00	CLOSED	XX/XX/XX

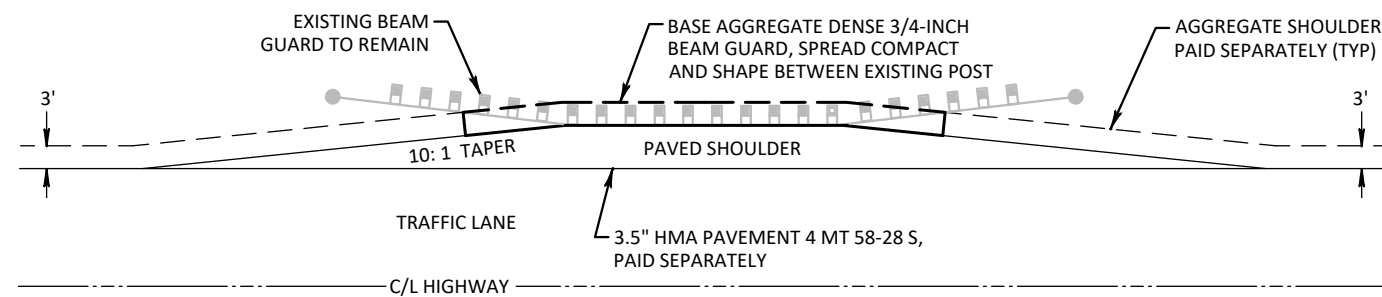
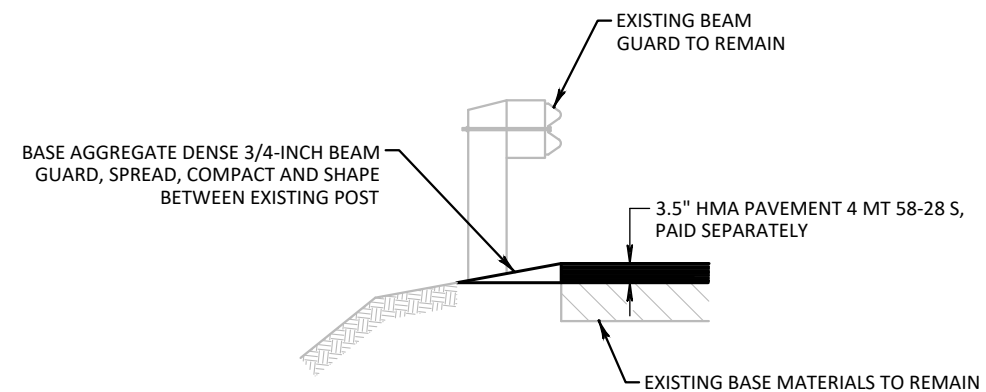
NOTE: FOLLOW STANDARD DETAIL DRAWINGS FOR ROAD CLOSURES.

PCMS MESSAGE



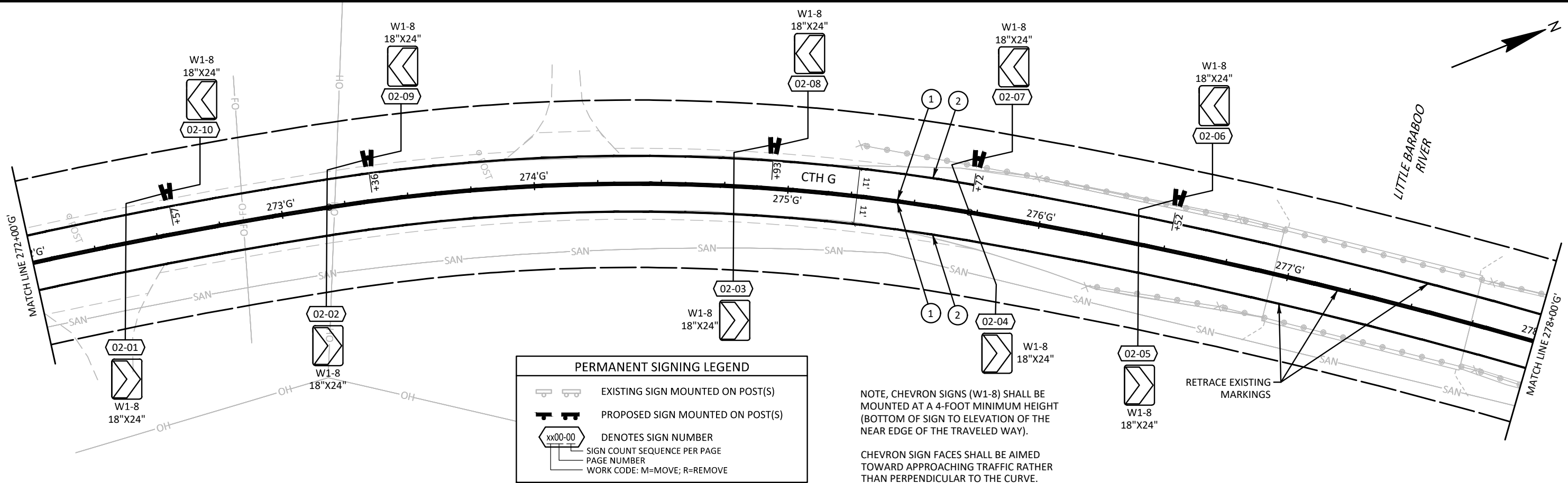
\* = INCIDENTAL TO BID ITEM  
"BASE REPAIR FOR CIR LAYER"

BASE REPAIR FOR CIR LAYER



BASE AGGREGATE DENSE 3/4-INCH BEAM GUARD





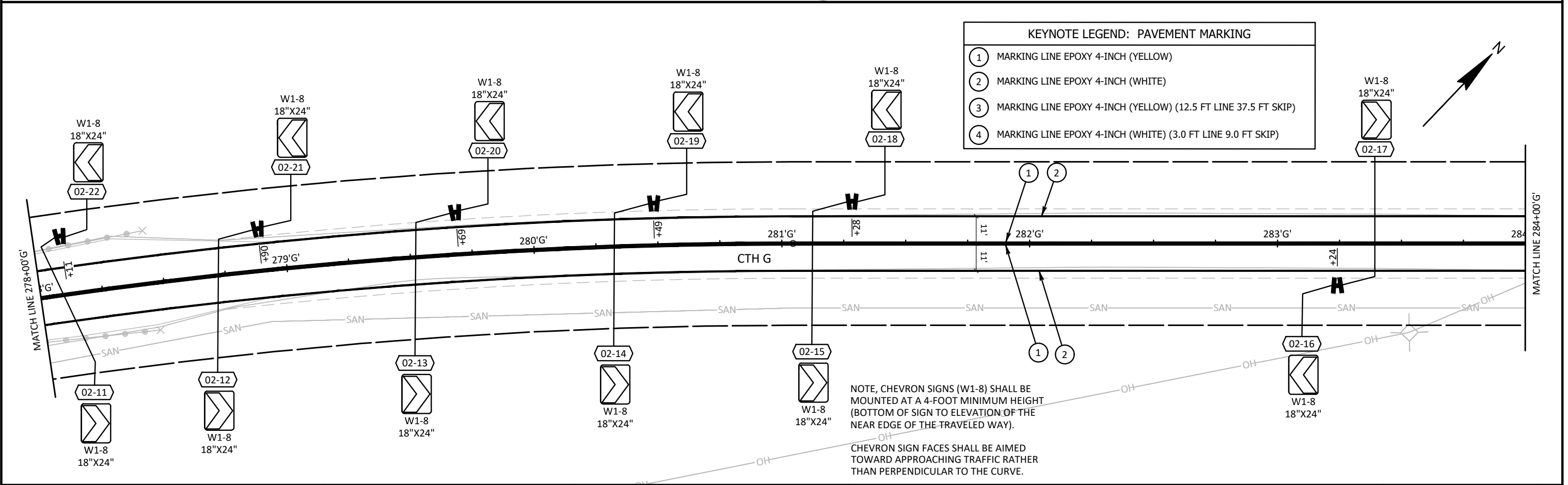
**PERMANENT SIGNING LEGEND**

- EXISTING SIGN MOUNTED ON POST(S)
- PROPOSED SIGN MOUNTED ON POST(S)
- DENOTES SIGN NUMBER
- SIGN COUNT SEQUENCE PER PAGE
- PAGE NUMBER
- WORK CODE: M=MOVE; R=REMOVE

NOTE, CHEVRON SIGNS (W1-8) SHALL BE MOUNTED AT A 4-FOOT MINIMUM HEIGHT (BOTTOM OF SIGN TO ELEVATION OF THE NEAR EDGE OF THE TRAVELED WAY).

CHEVRON SIGN FACES SHALL BE AIMED TOWARD APPROACHING TRAFFIC RATHER THAN PERPENDICULAR TO THE CURVE.

RETRACE EXISTING MARKINGS



**KEYNOTE LEGEND: PAVEMENT MARKING**

- ① MARKING LINE EPOXY 4-INCH (YELLOW)
- ② MARKING LINE EPOXY 4-INCH (WHITE)
- ③ MARKING LINE EPOXY 4-INCH (YELLOW) (12.5 FT LINE 37.5 FT SKIP)
- ④ MARKING LINE EPOXY 4-INCH (WHITE) (3.0 FT LINE 9.0 FT SKIP)

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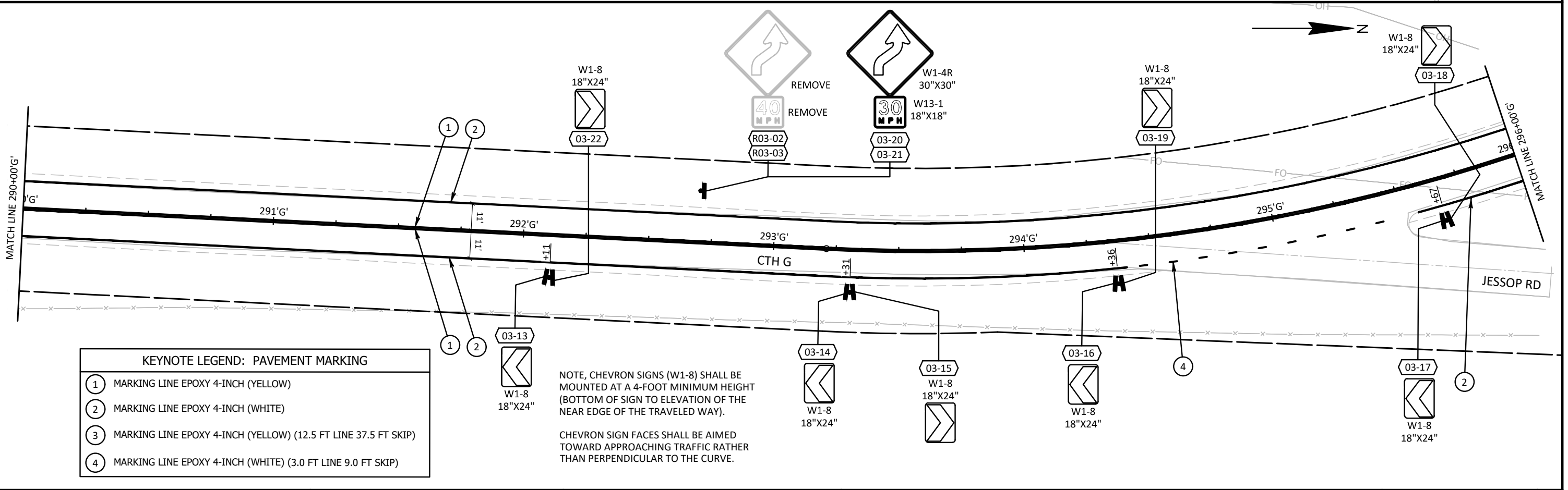
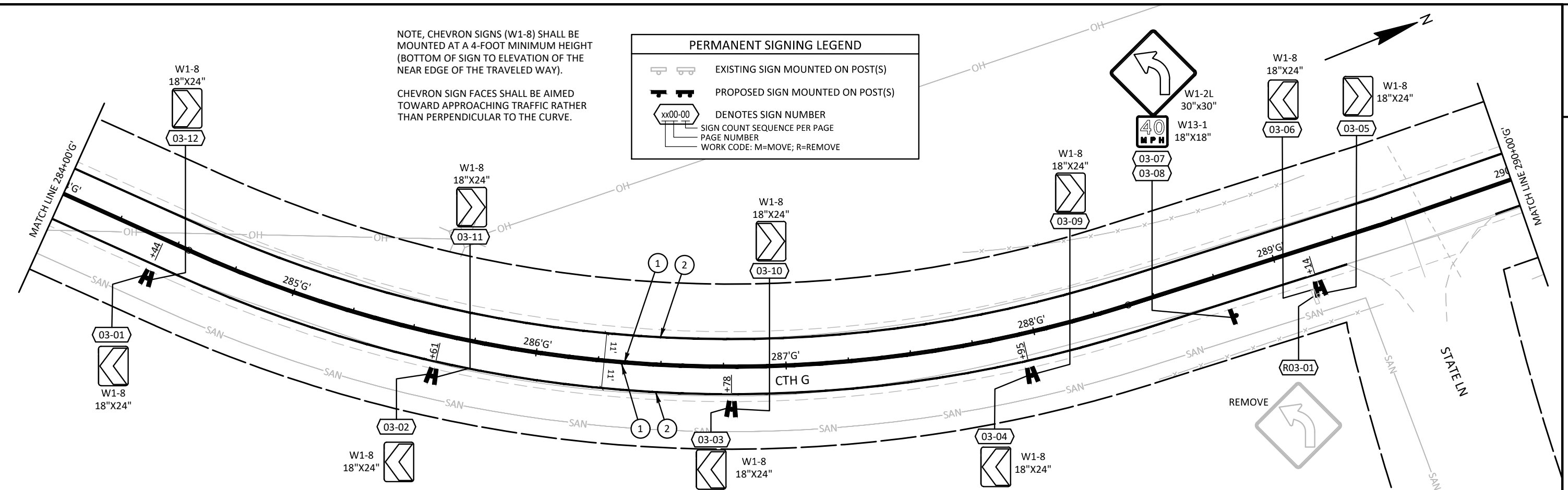
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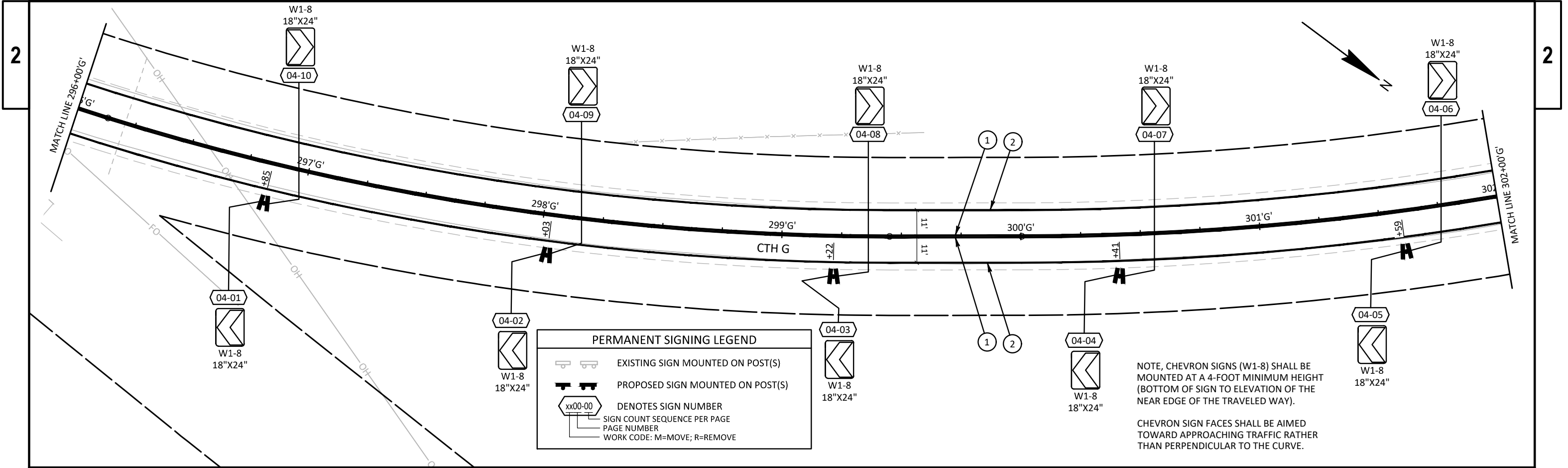


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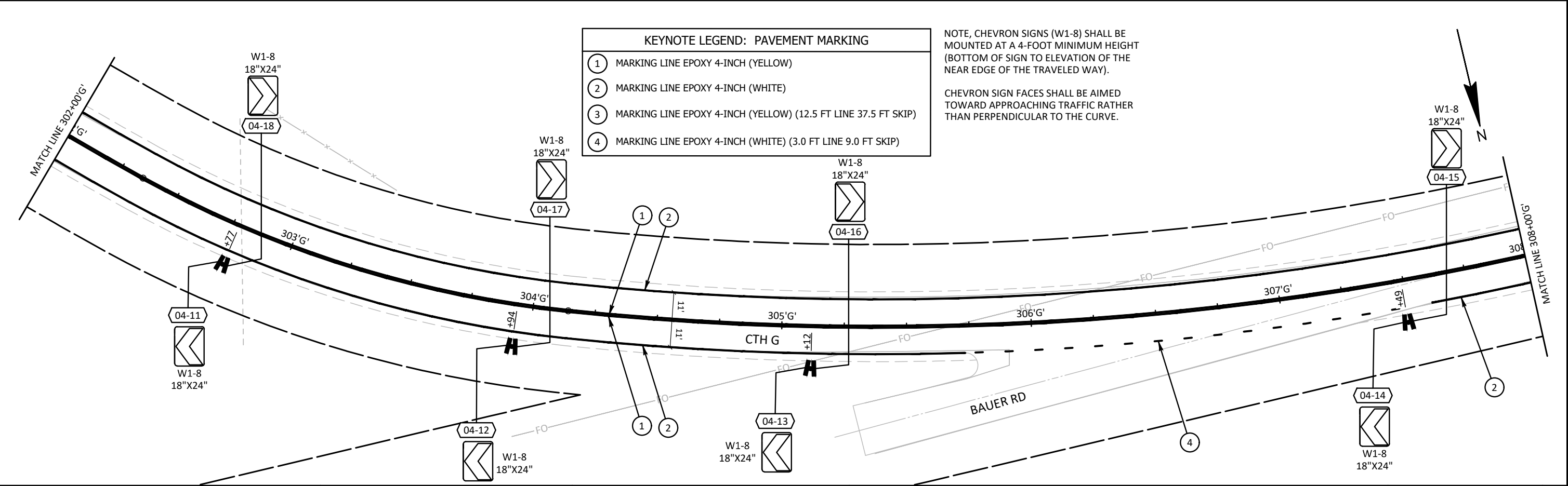
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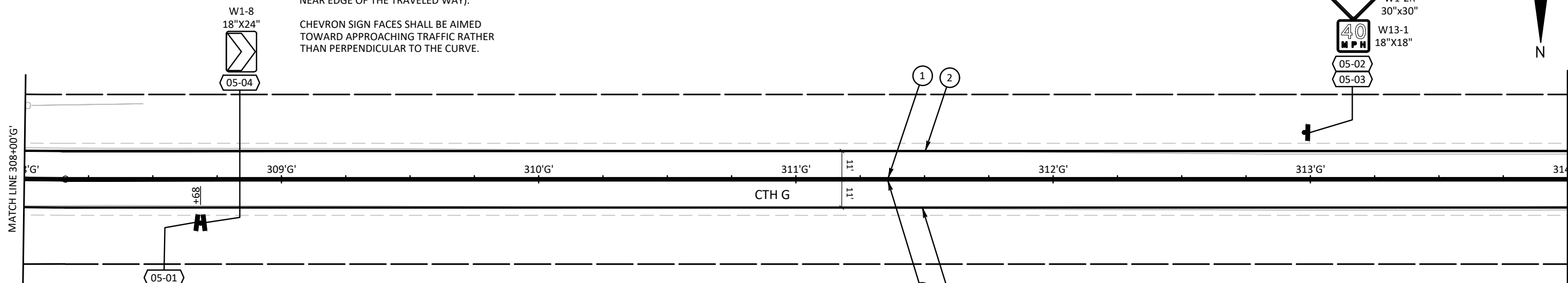
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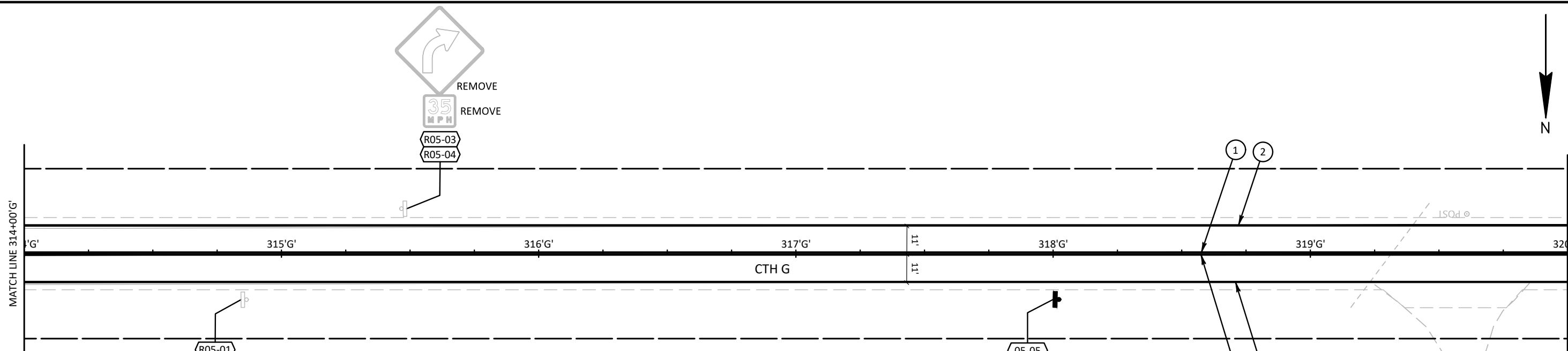
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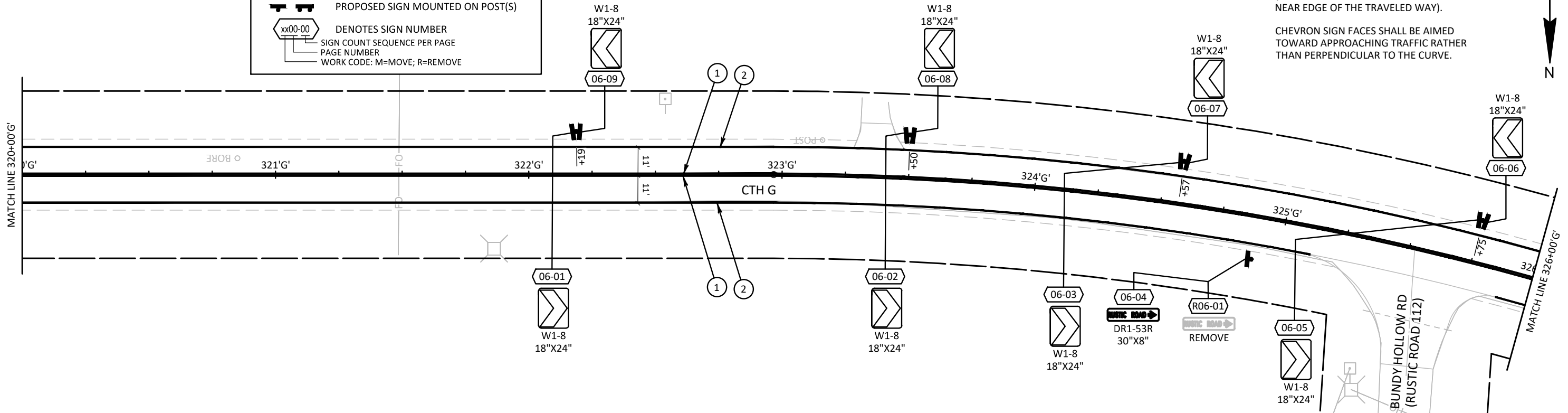


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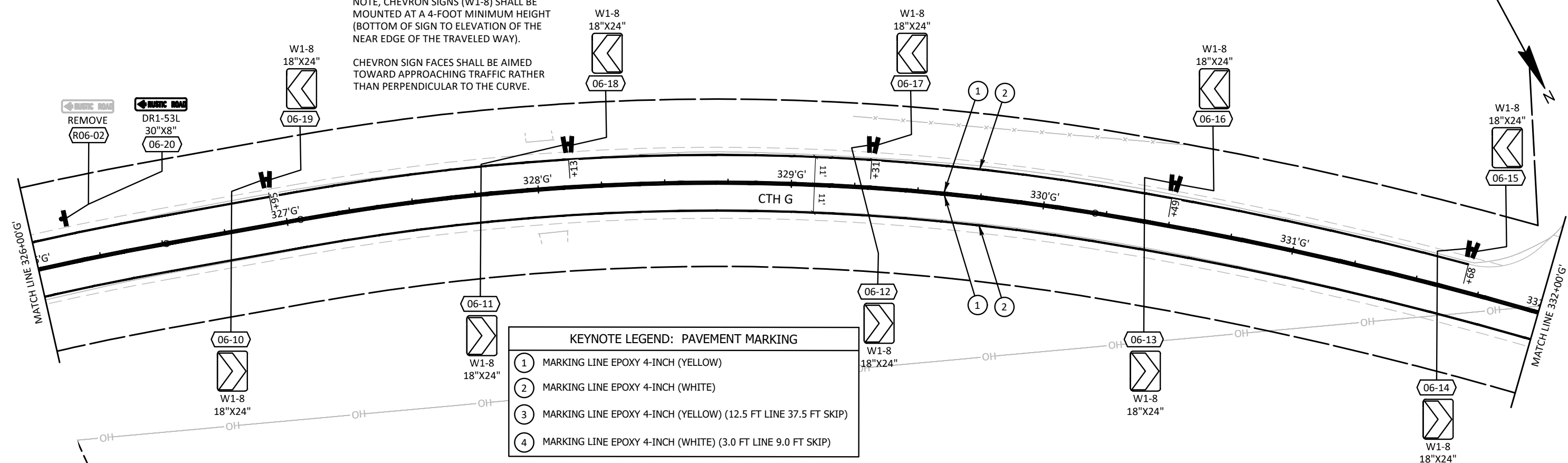
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- ③ MARKING LINE EPOXY 4-INCH (YELLOW) (12.5 FT LINE 37.5 FT SKIP)
- ④ MARKING LINE EPOXY 4-INCH (WHITE) (3.0 FT LINE 9.0 FT SKIP)

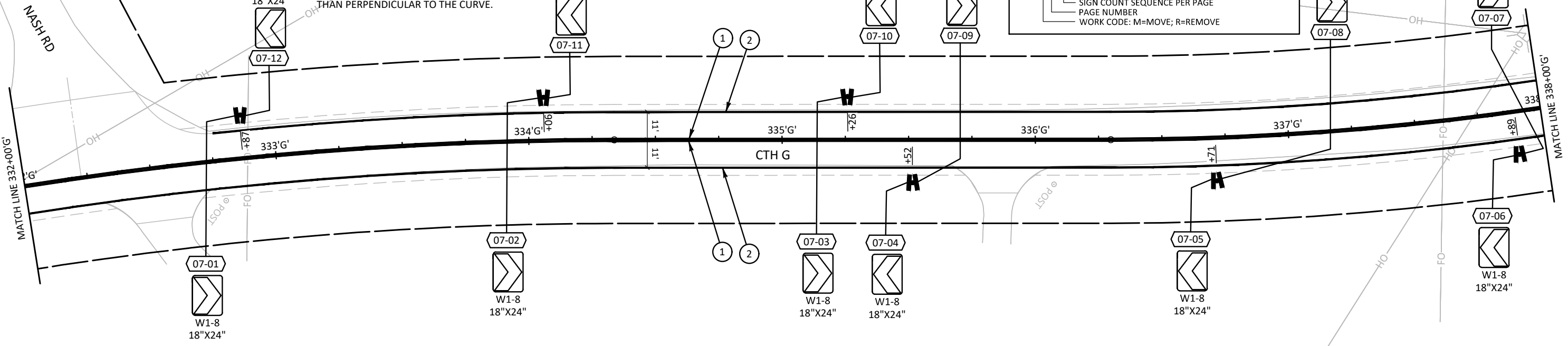


NOTE, CHEVRON SIGNS (W1-8) SHALL BE MOUNTED AT A 4-FOOT MINIMUM HEIGHT (BOTTOM OF SIGN TO ELEVATION OF THE NEAR EDGE OF THE TRAVELED WAY).

CHEVRON SIGN FACES SHALL BE AIMED TOWARD APPROACHING TRAFFIC RATHER THAN PERPENDICULAR TO THE CURVE.

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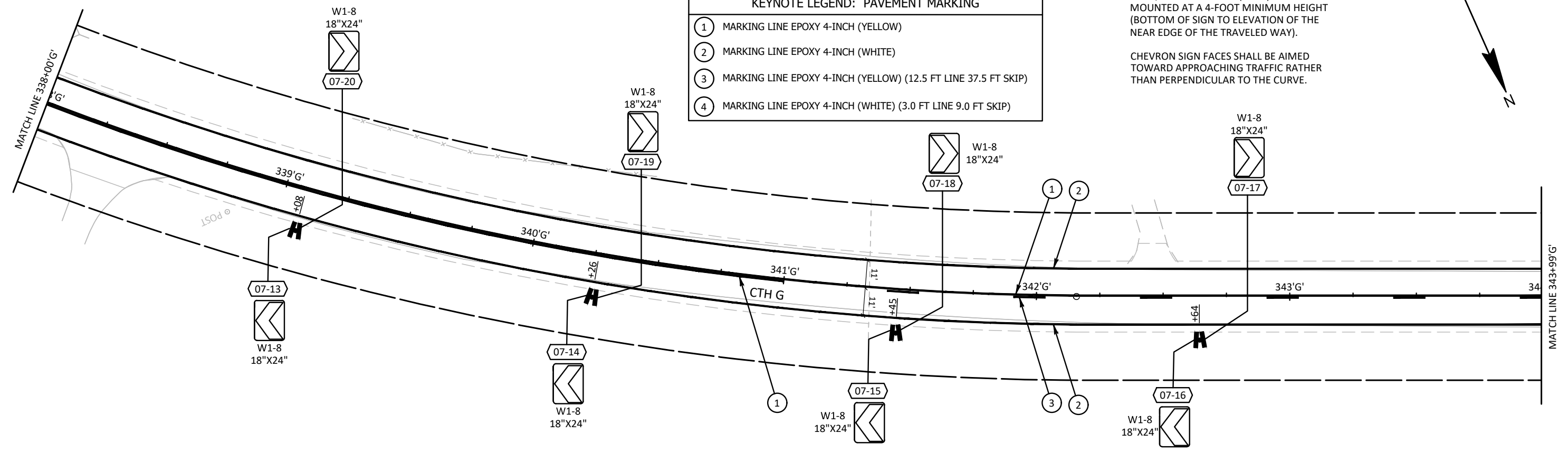


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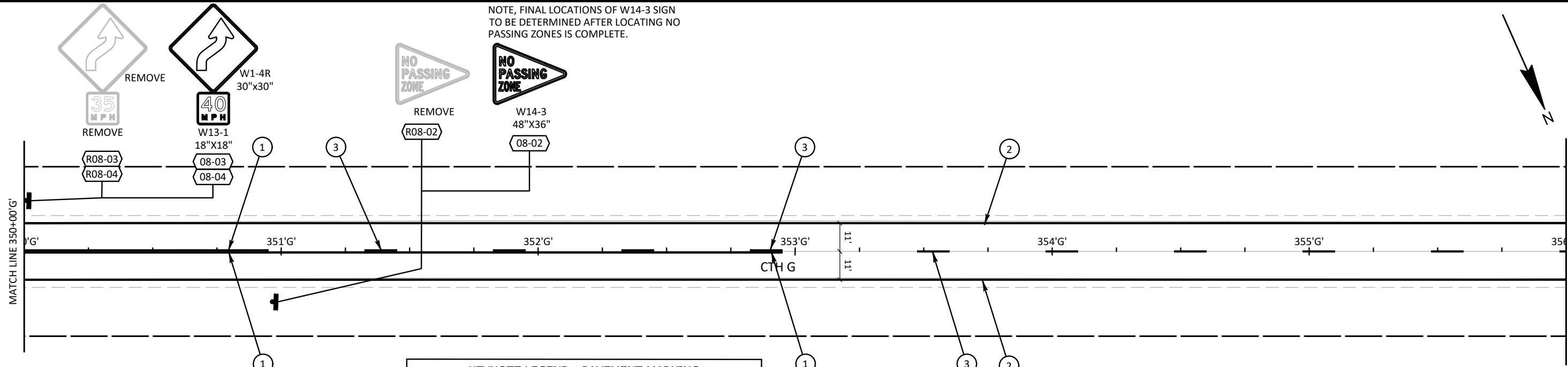
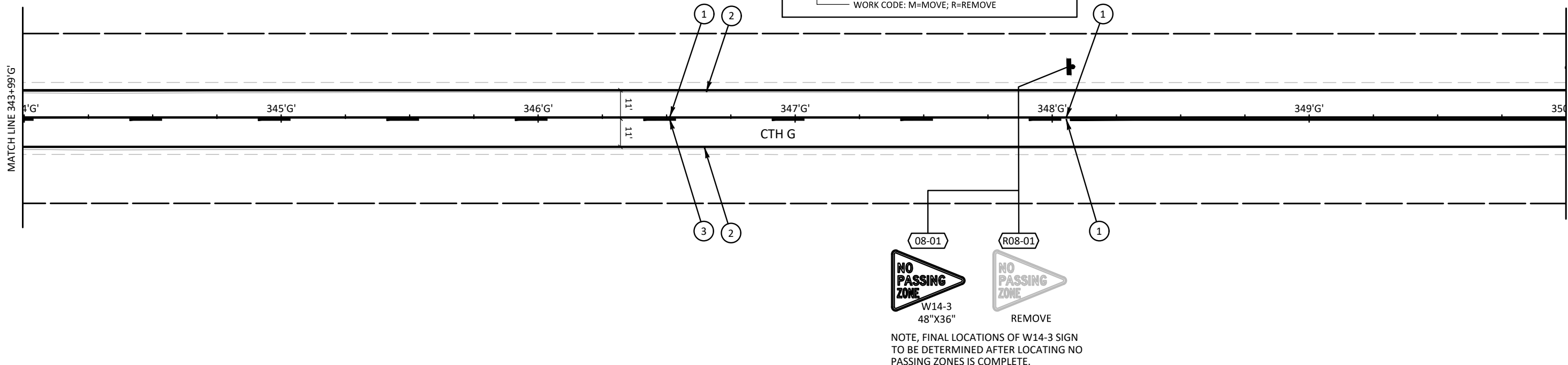
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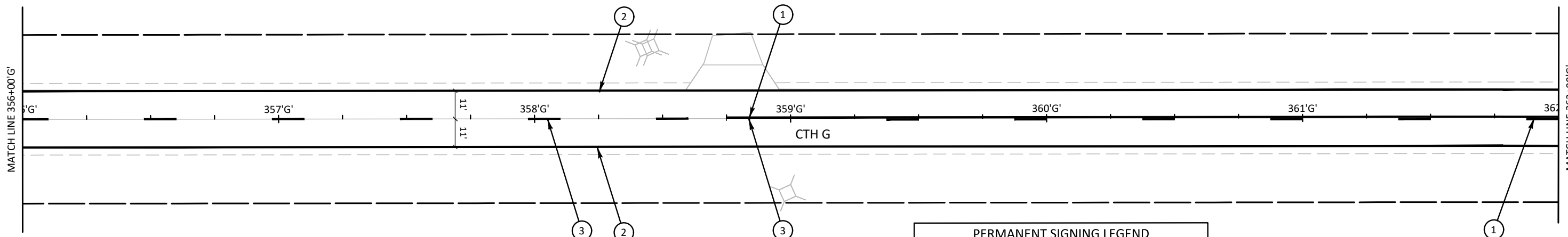
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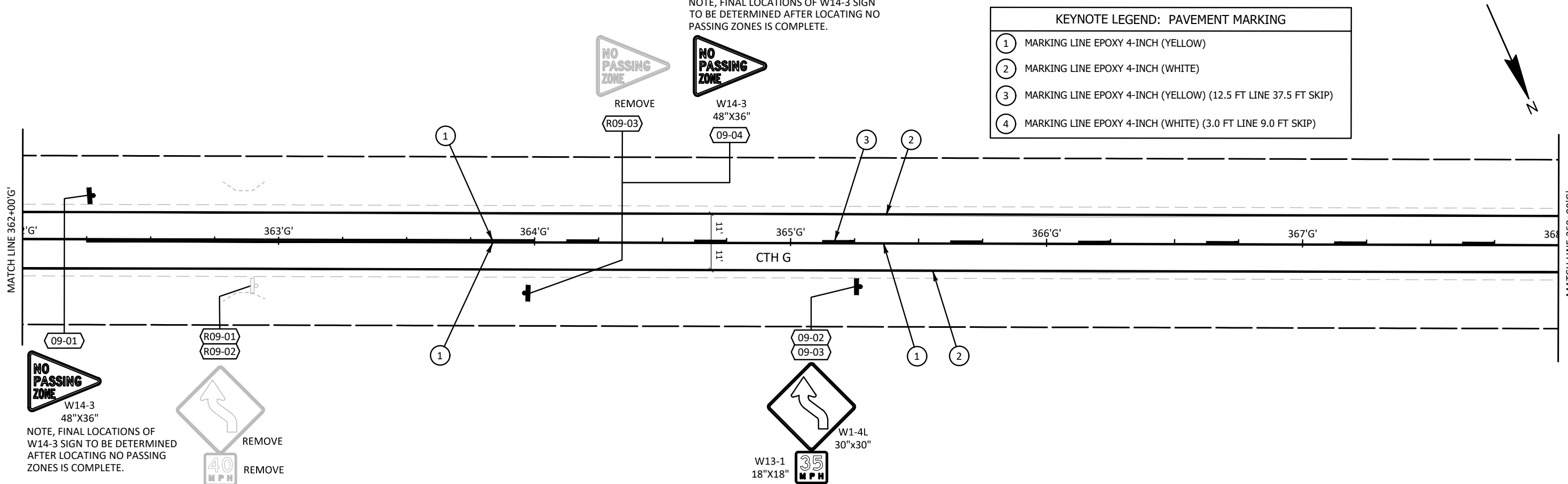
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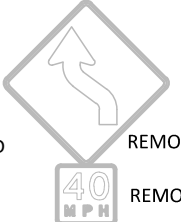


**KEYNOTE LEGEND: PAVEMENT MARKING**

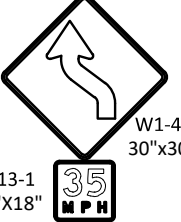
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W14-3  
48"X36"  
NOTE, FINAL LOCATIONS OF W14-3 SIGN TO BE DETERMINED AFTER LOCATING NO PASSING ZONES IS COMPLETE.



REMOVE  
REMOVE



W1-4L  
30"X30"  
W13-1  
18"X18"



REMOVE  
R09-03



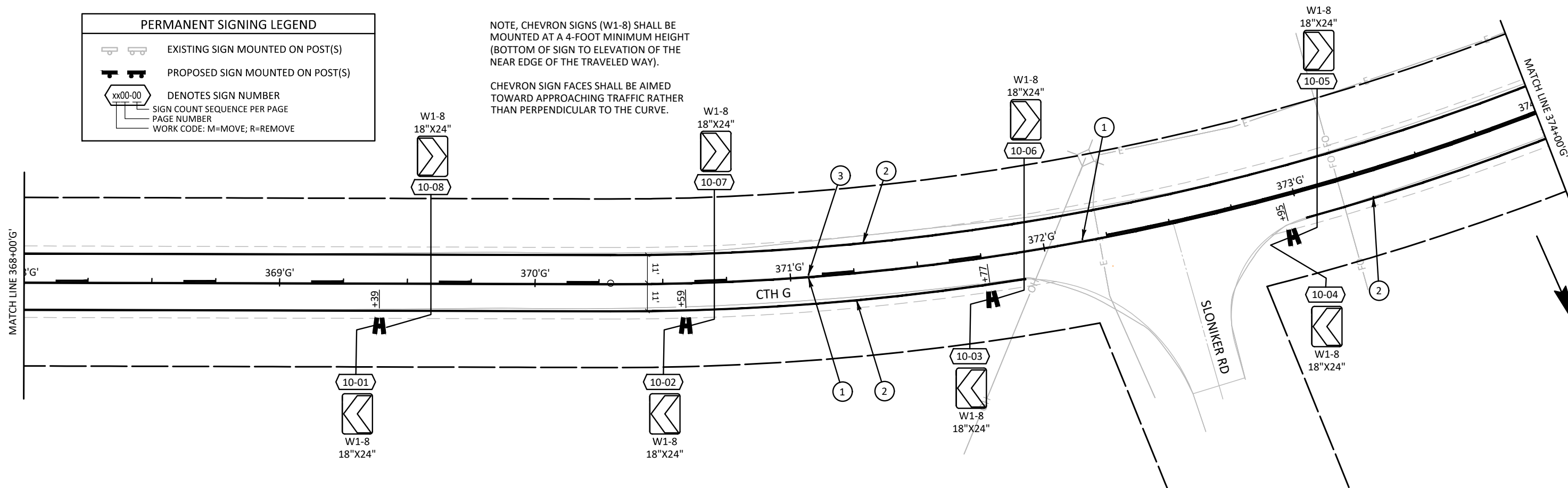
W14-3  
48"X36"  
09-04

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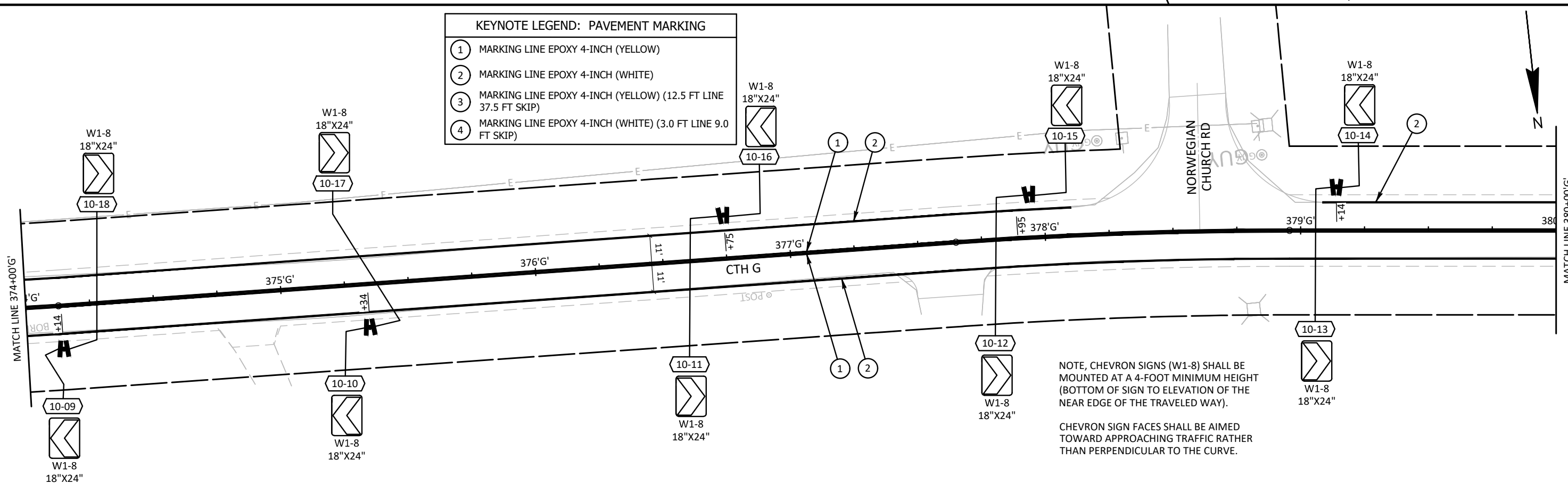
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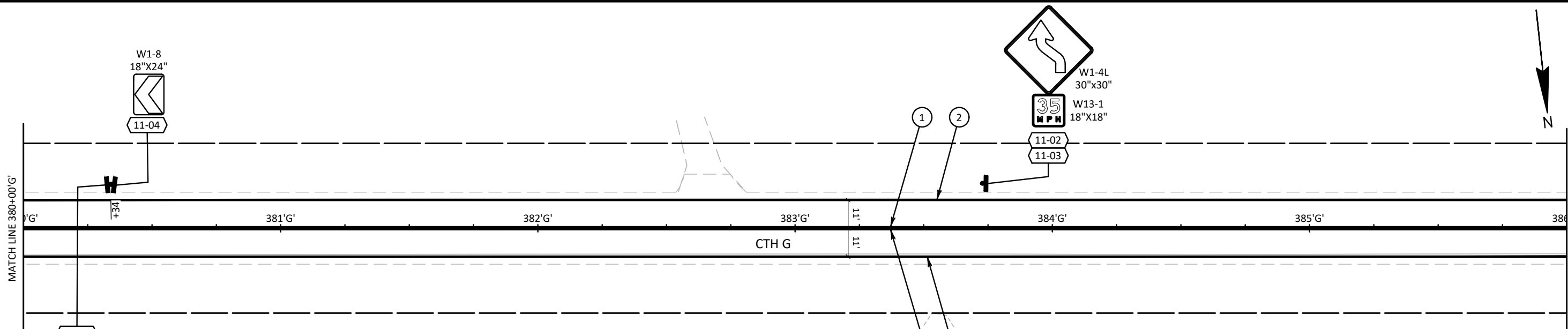
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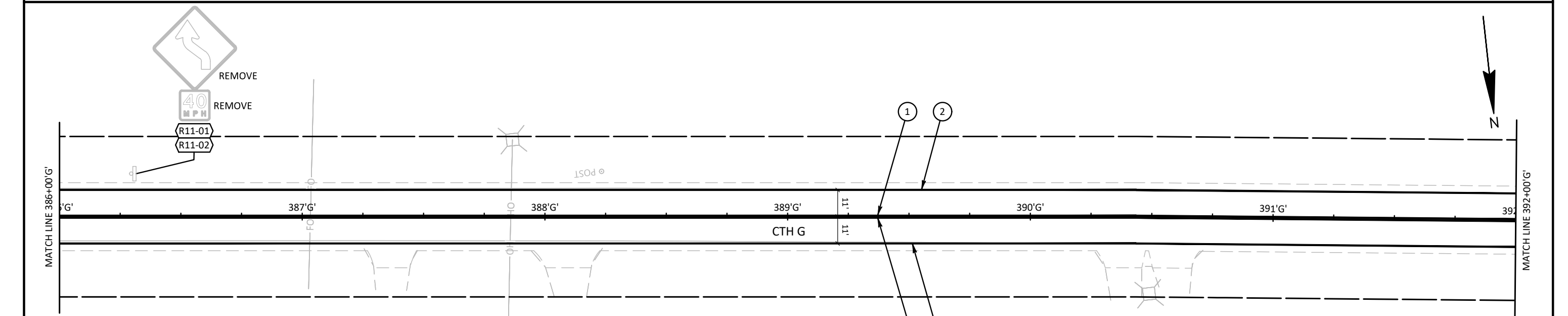
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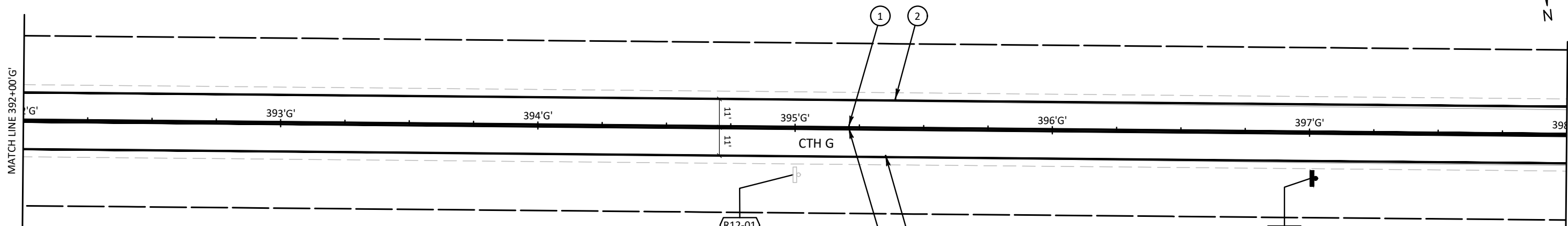
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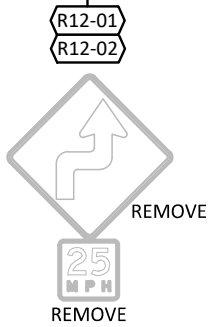


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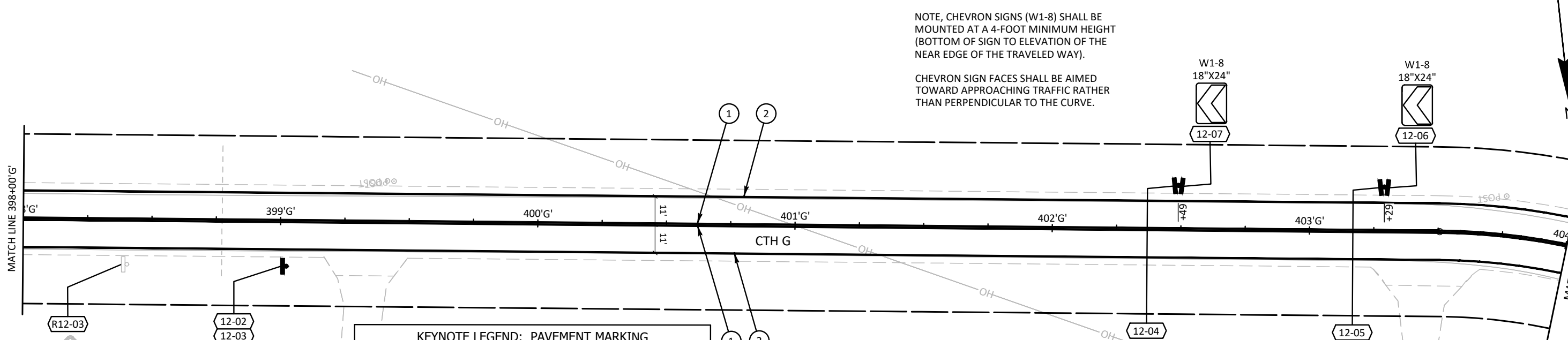
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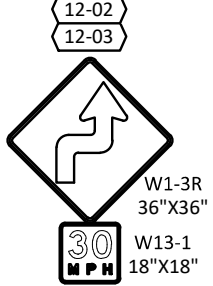
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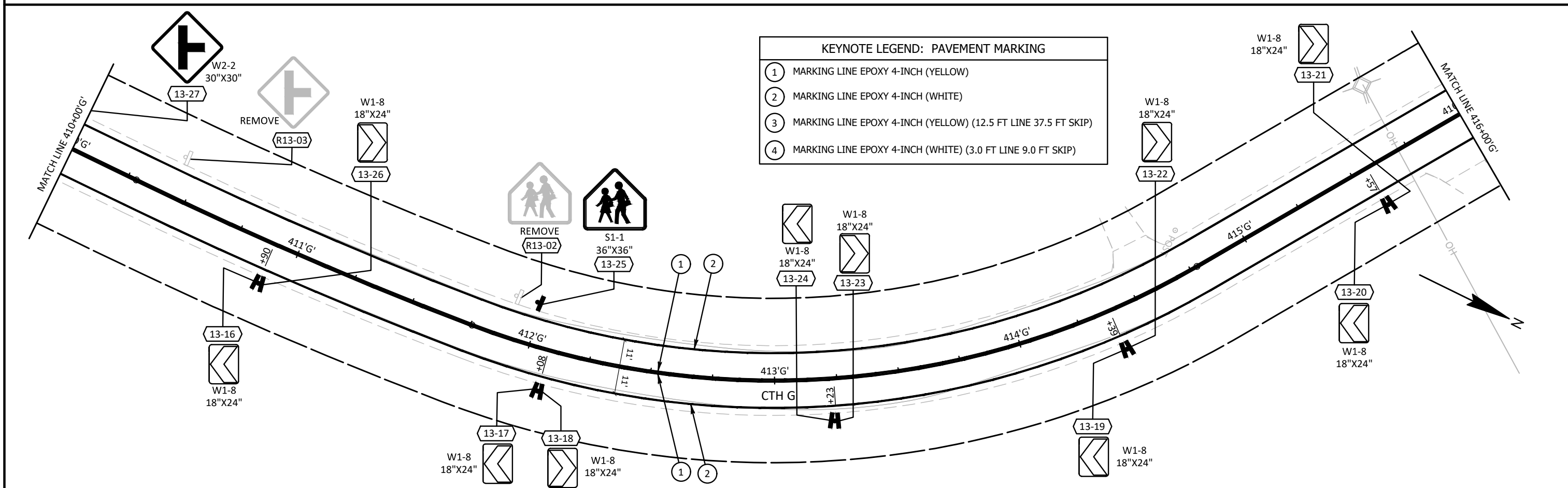
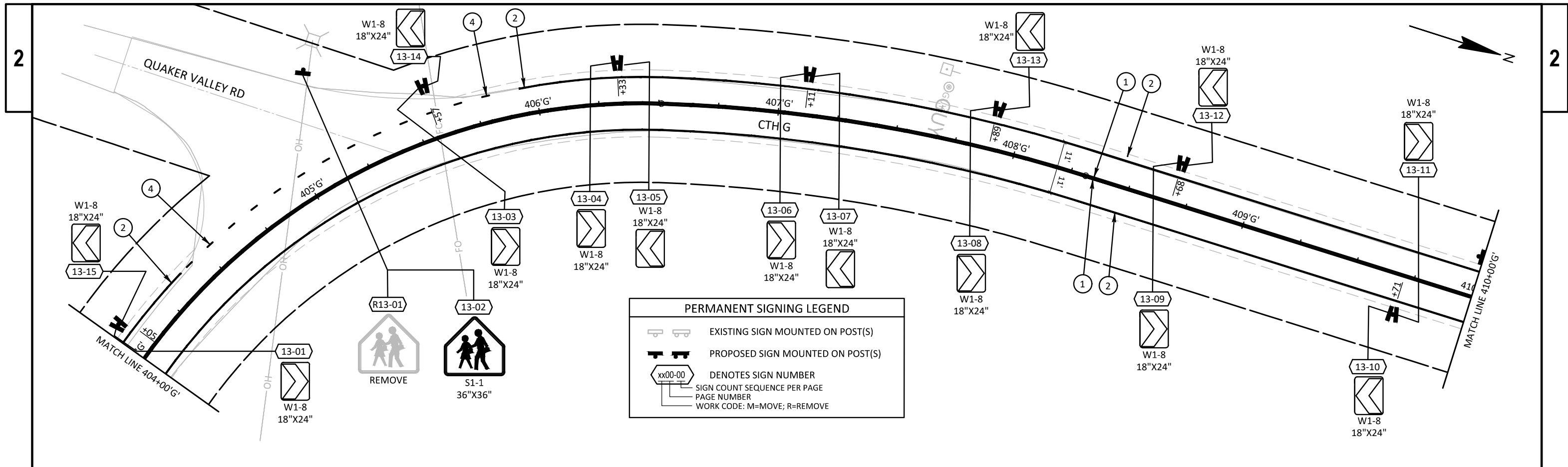


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PROJECT NO: 5908-00-74

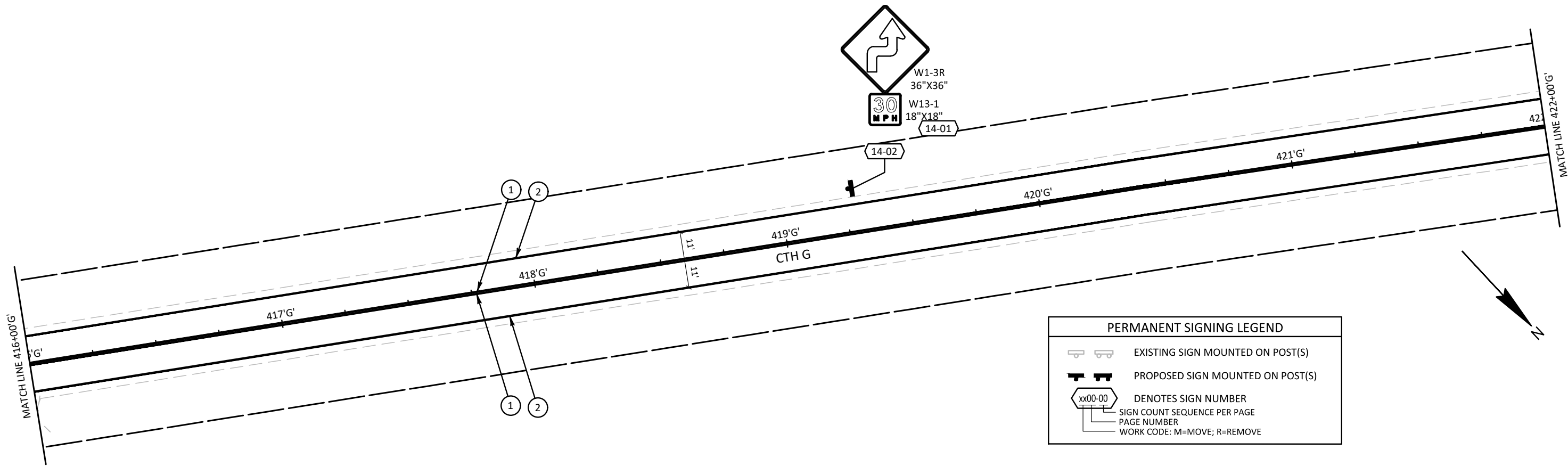
HWY: CTH G

COUNTY: SAUK

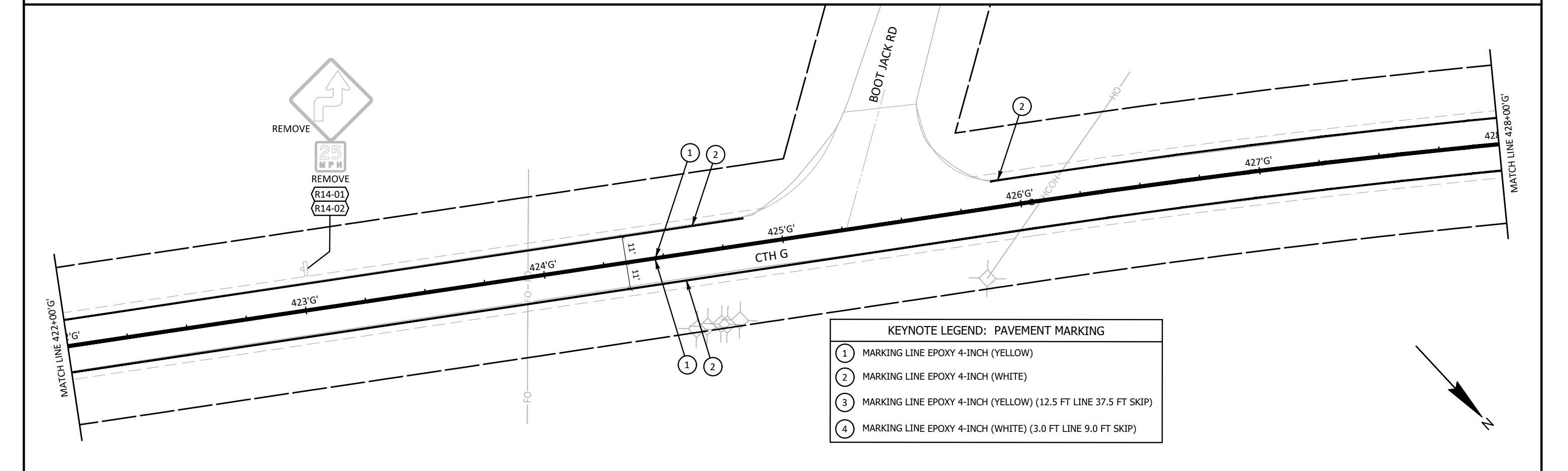
PERMANENT SIGNING AND PAVEMENT MARKING

SHEET

E



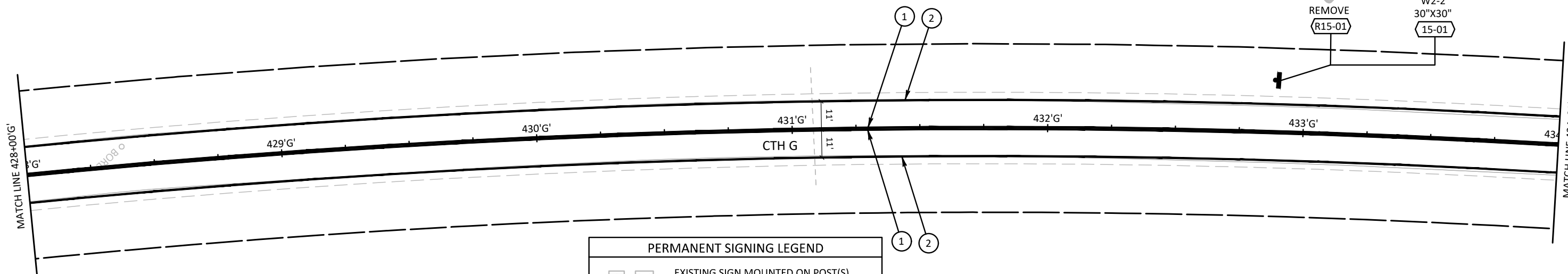
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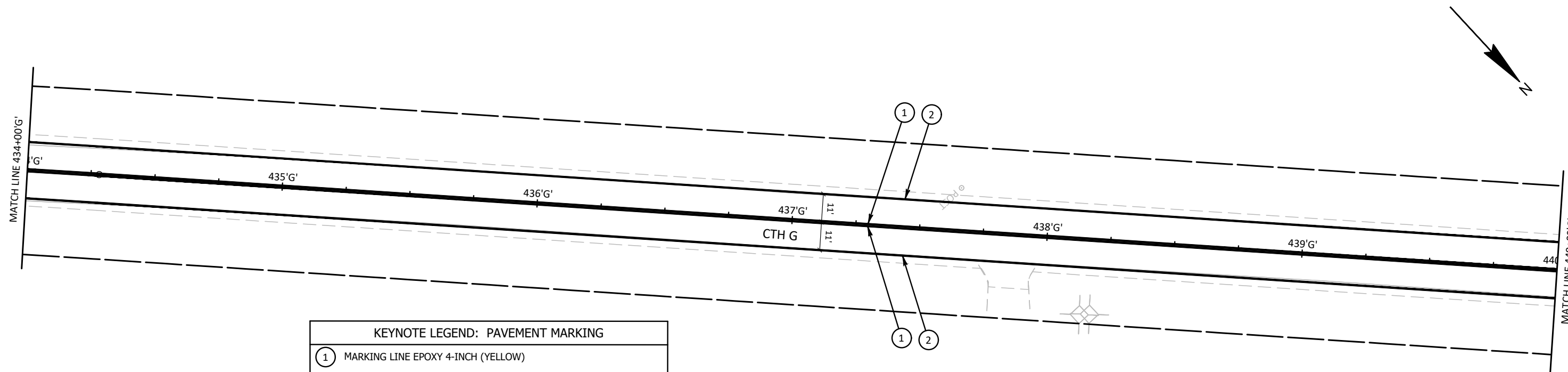
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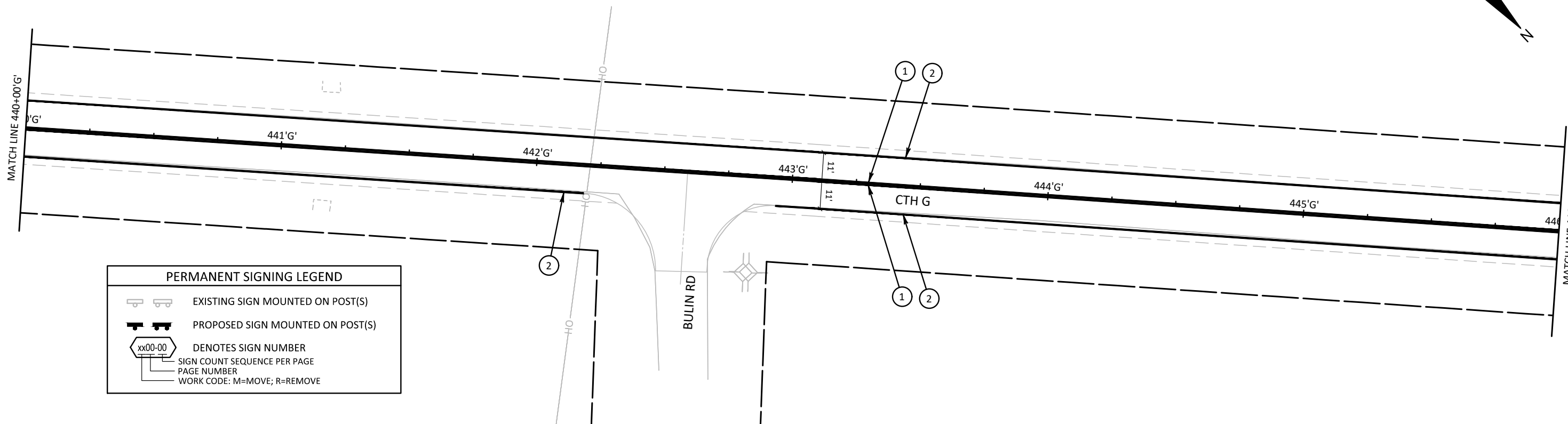




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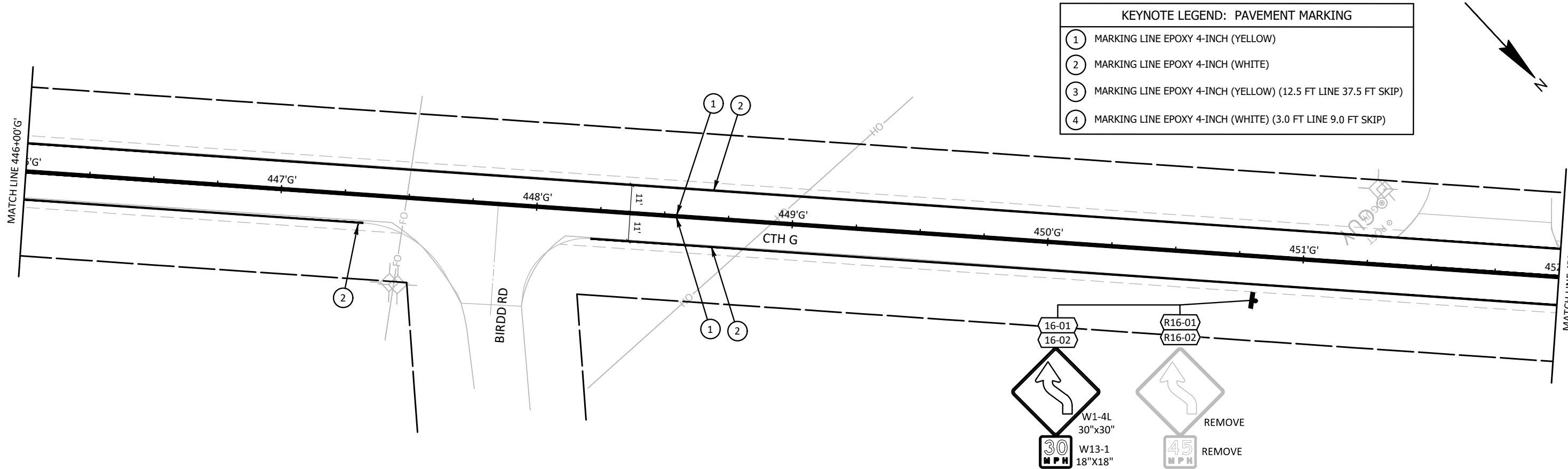


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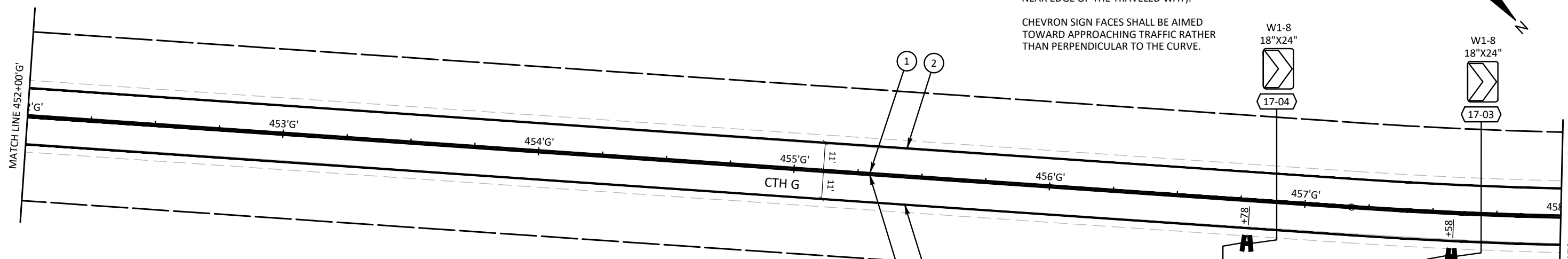
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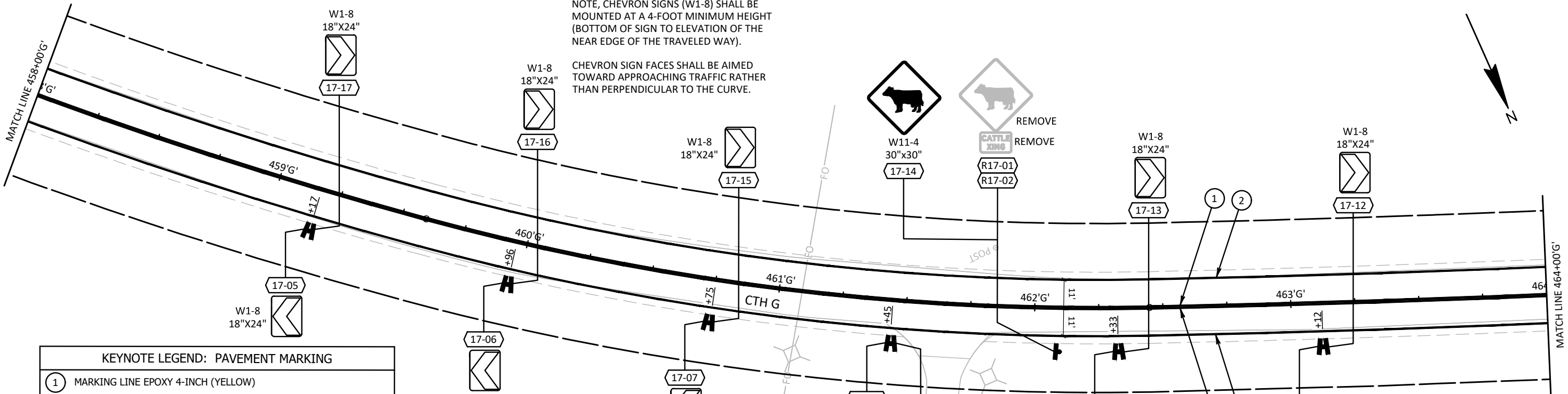
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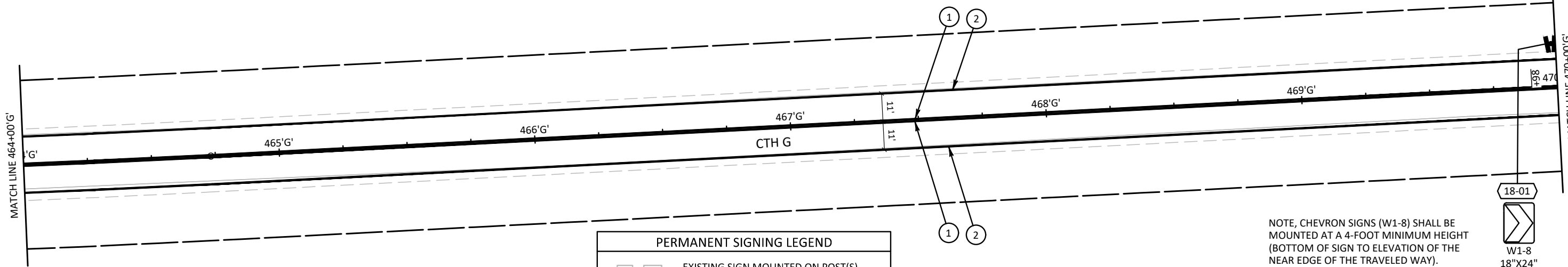
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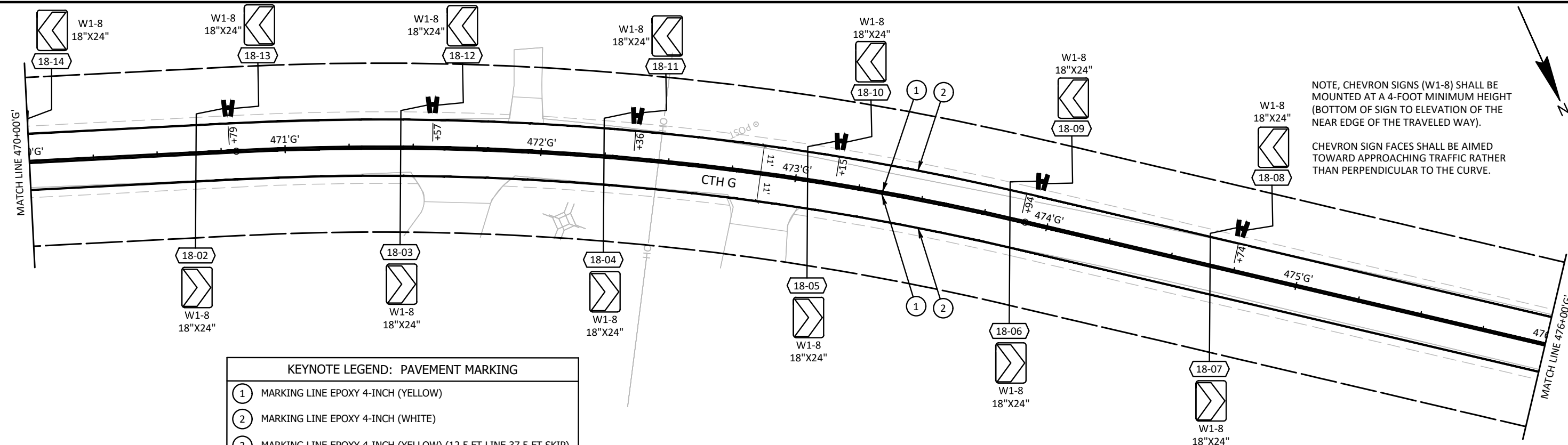
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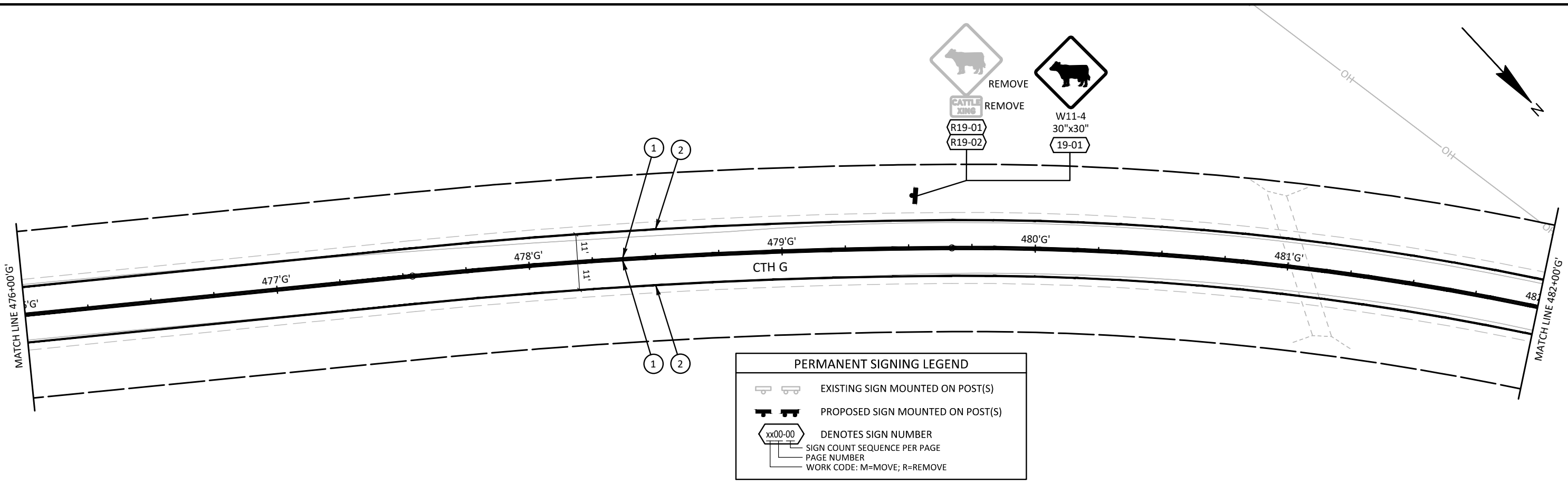
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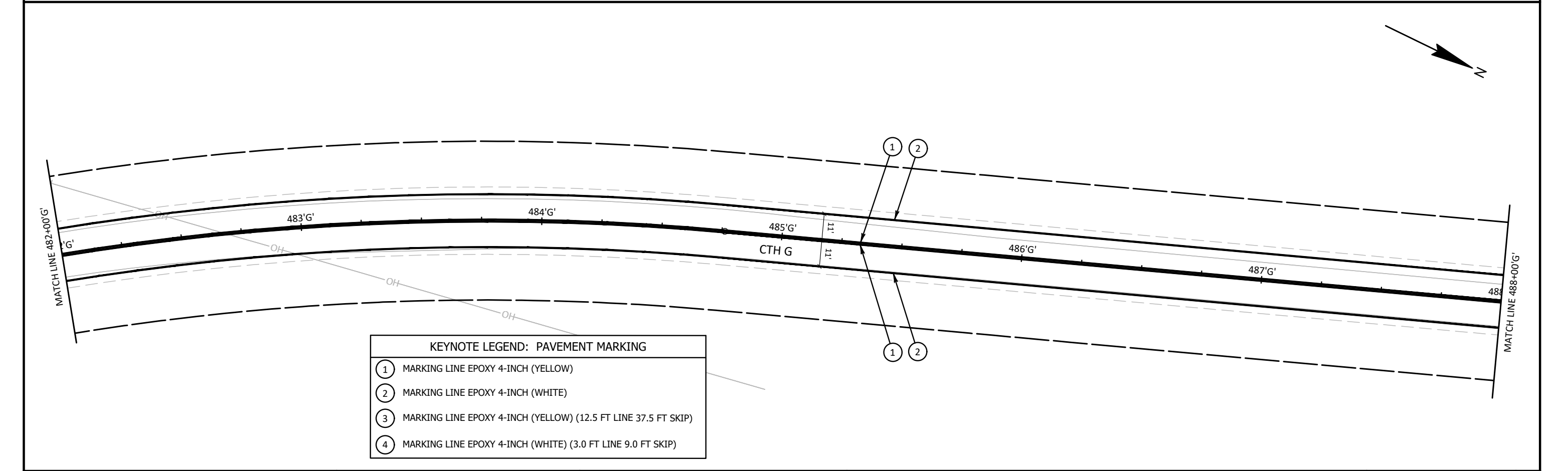
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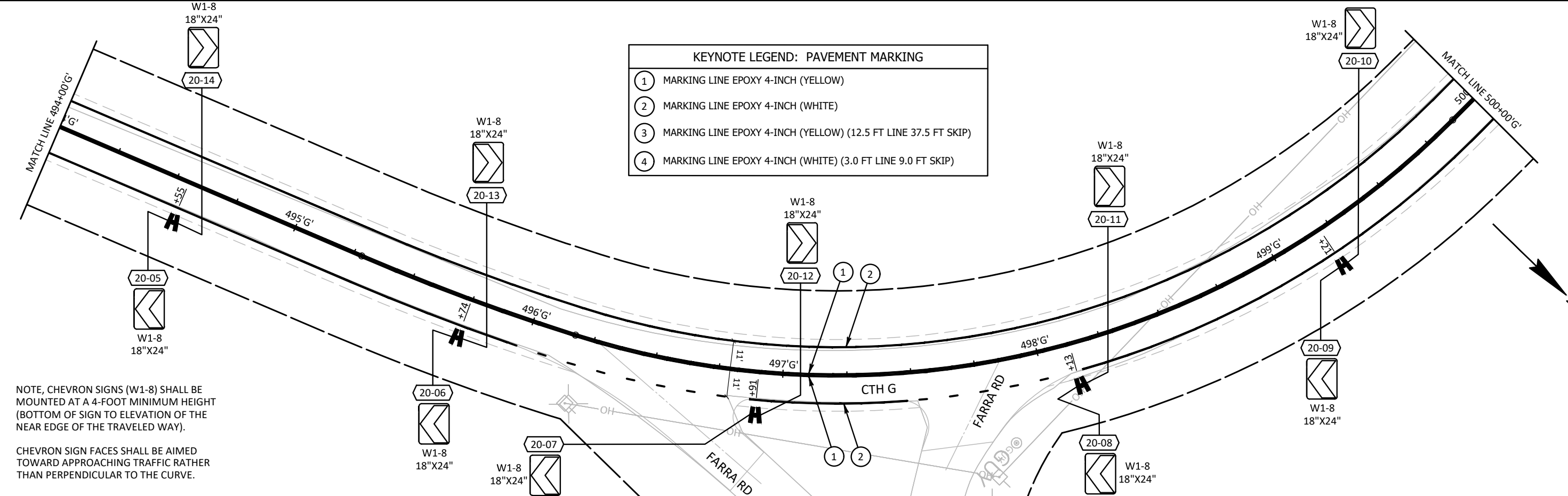
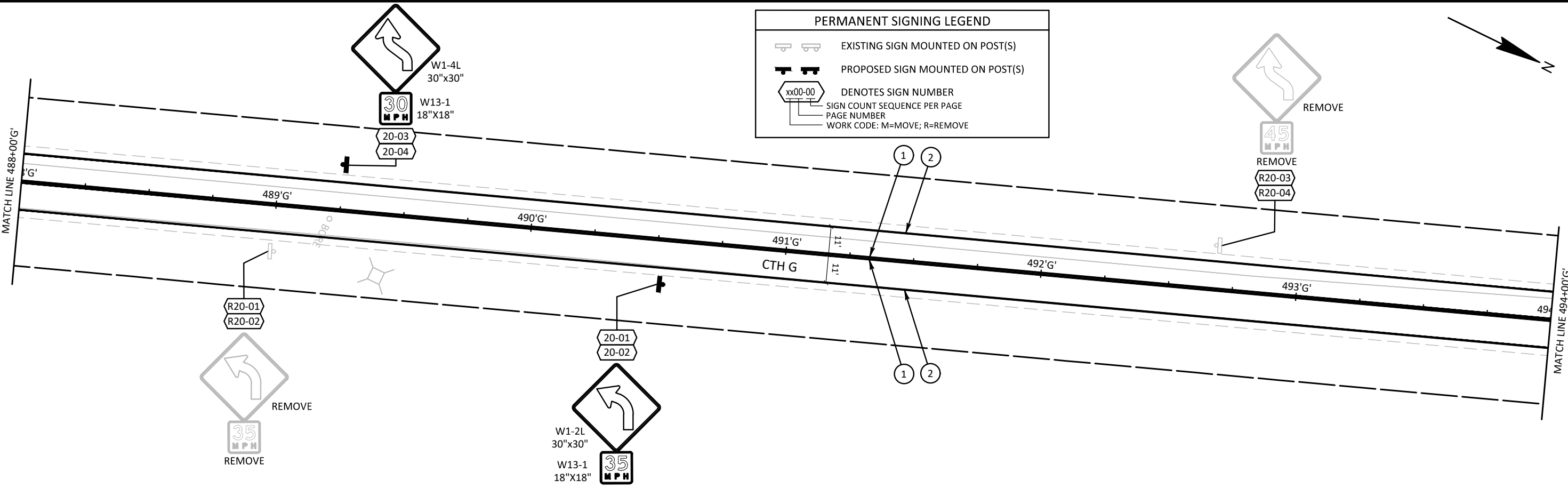
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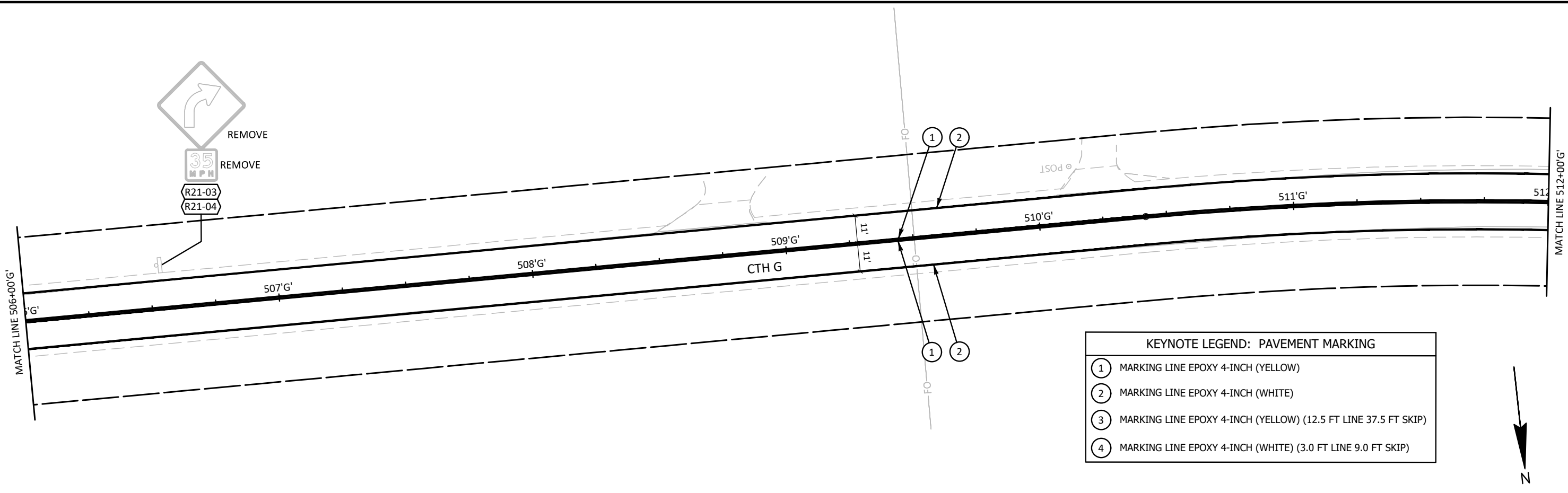
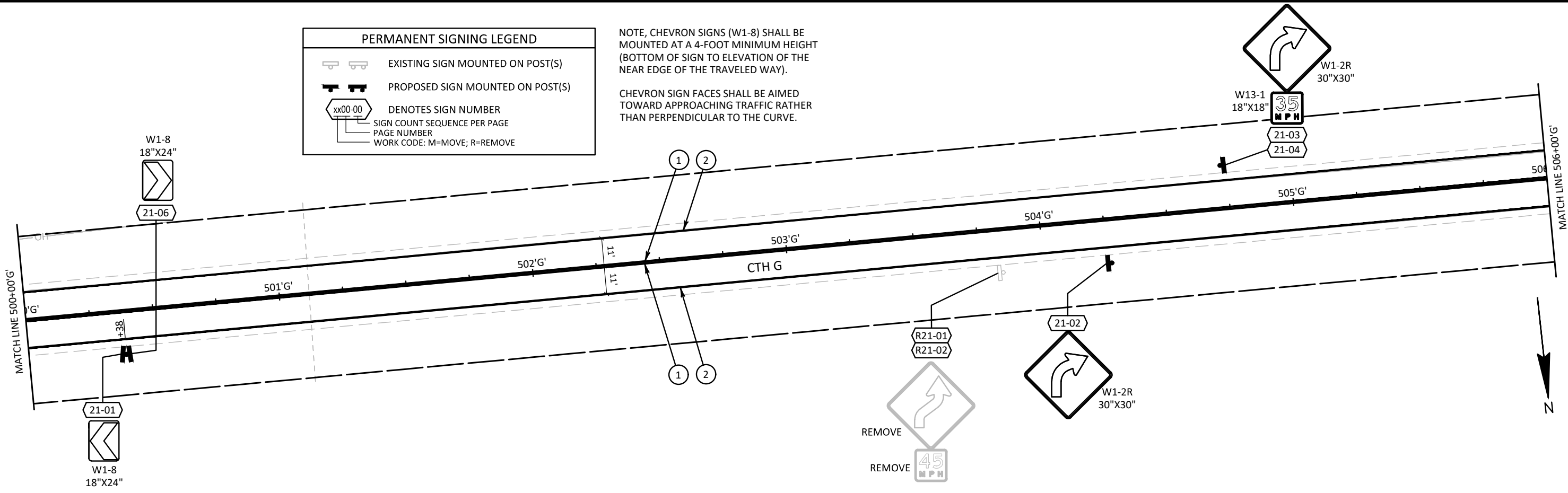


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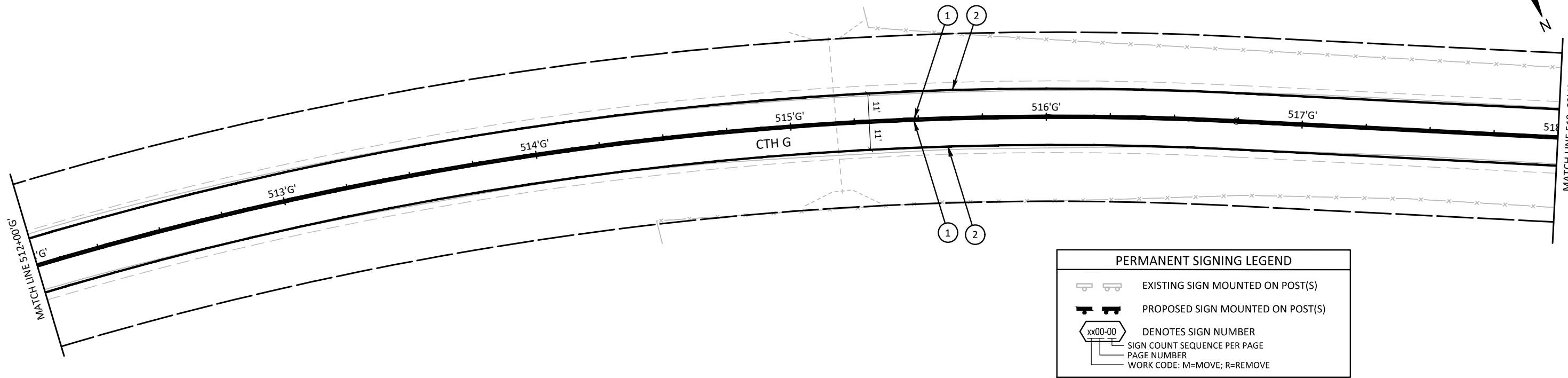
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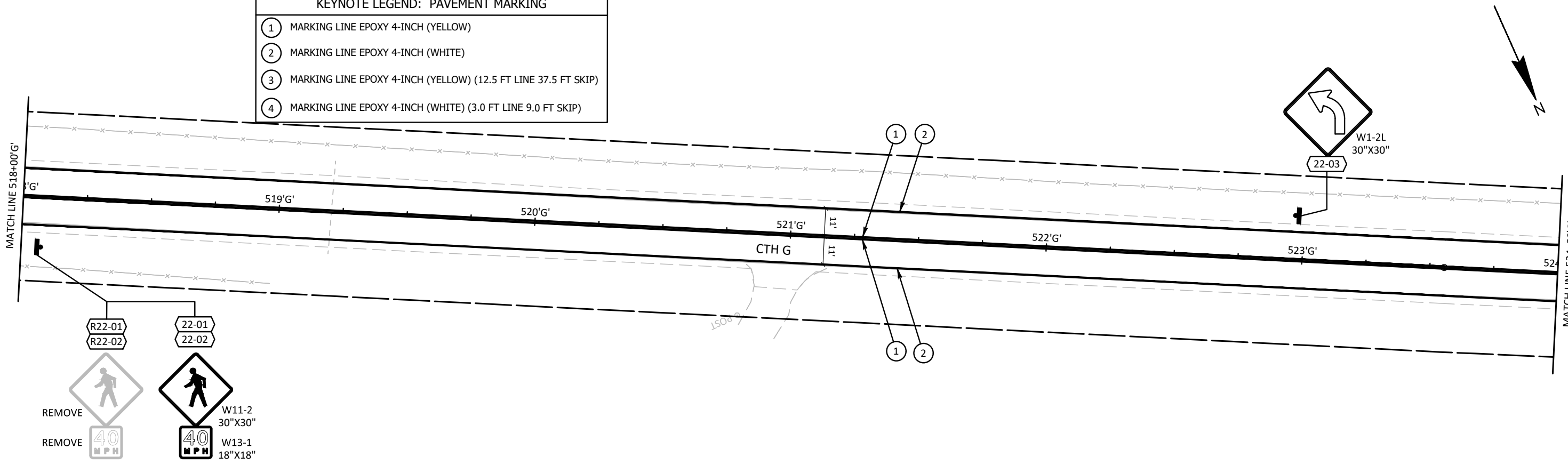


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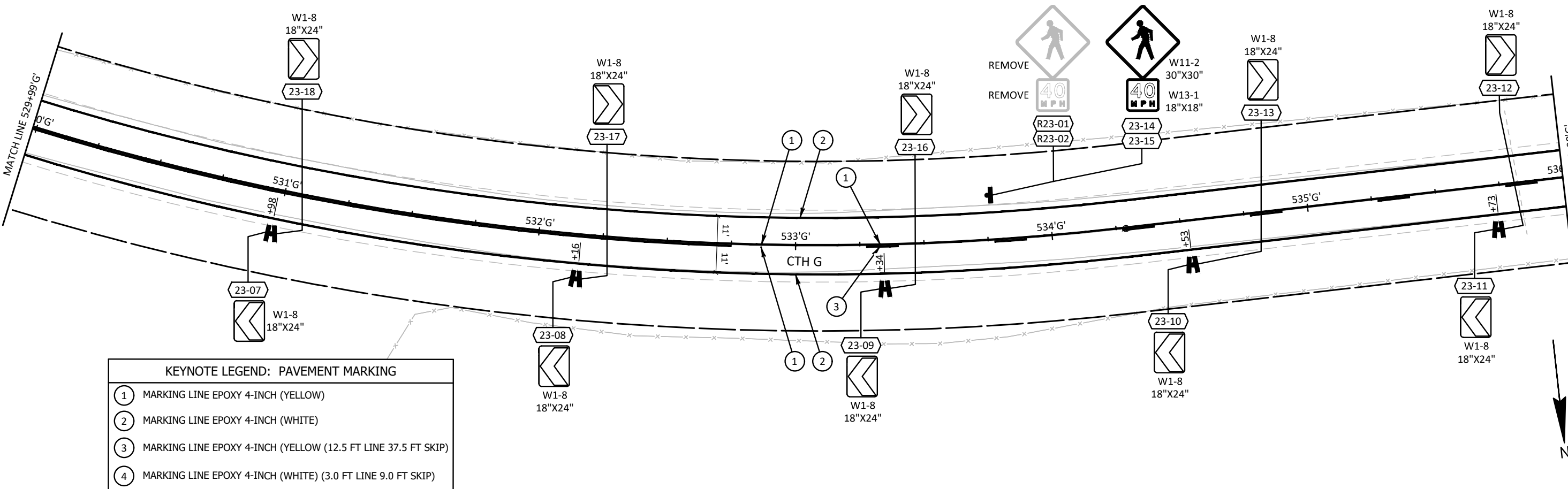
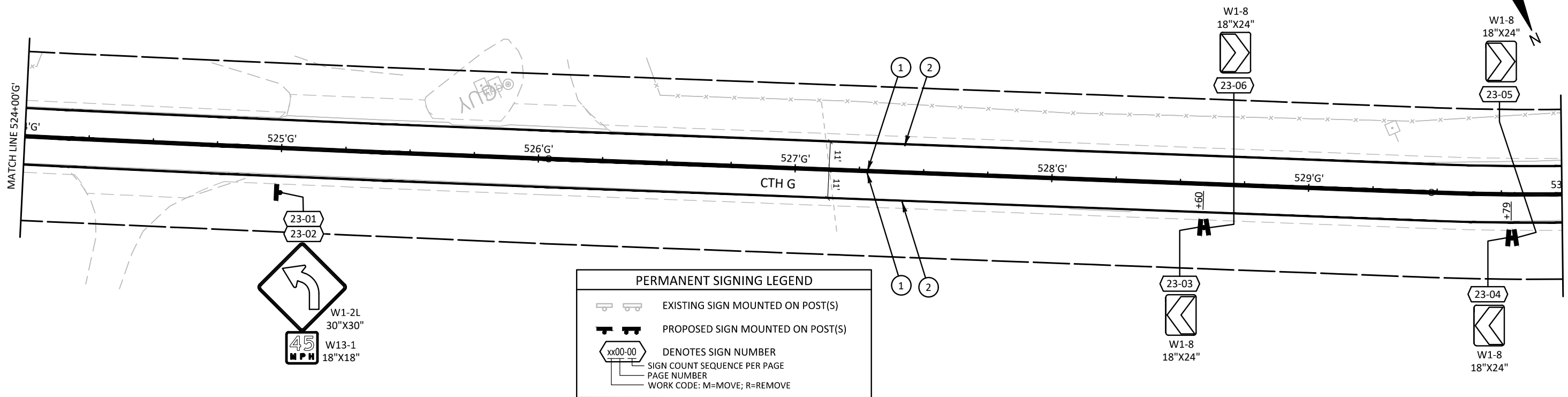


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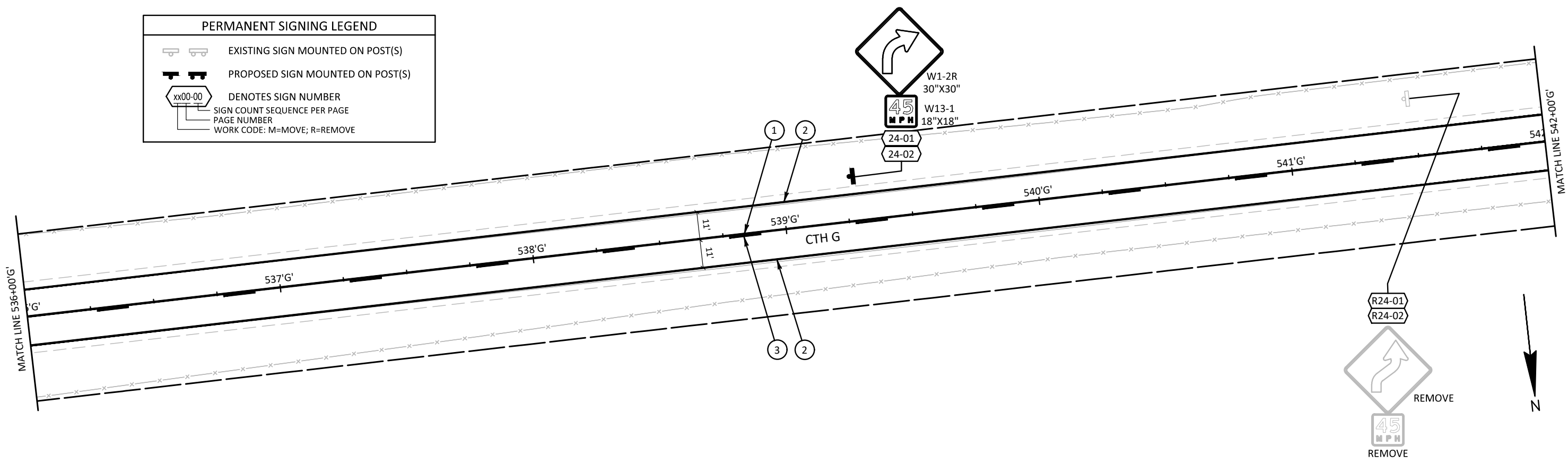
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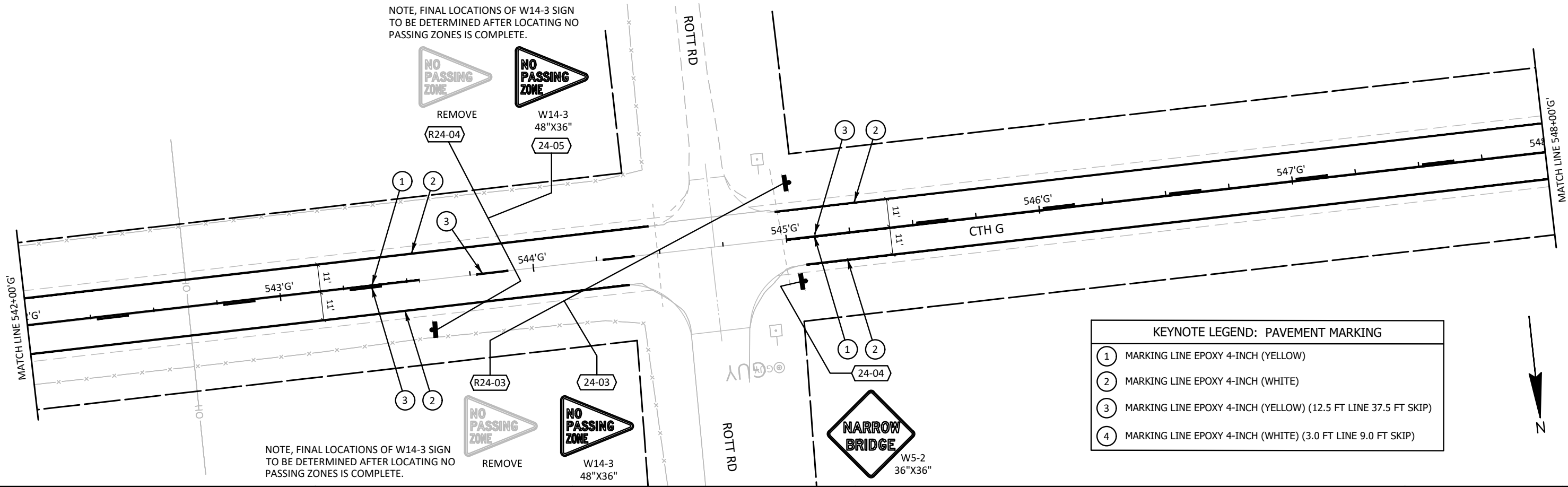




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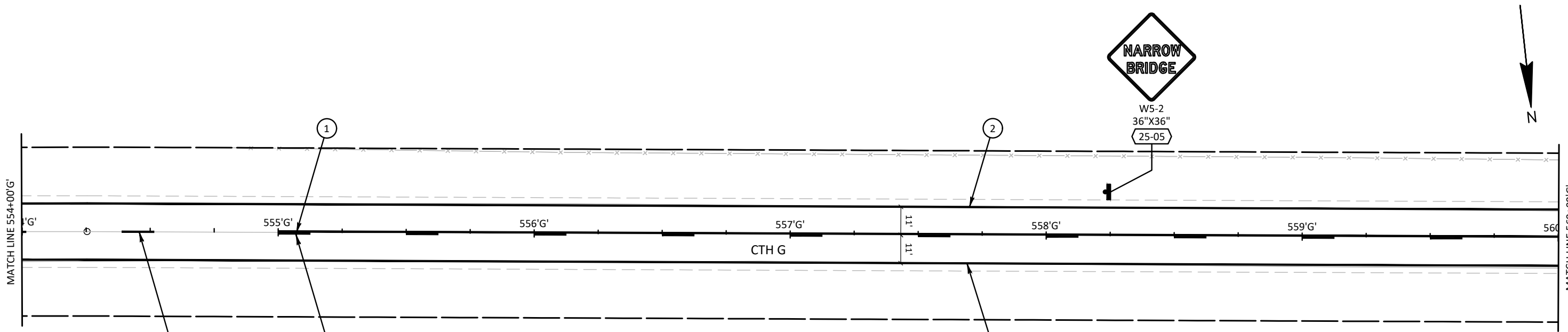
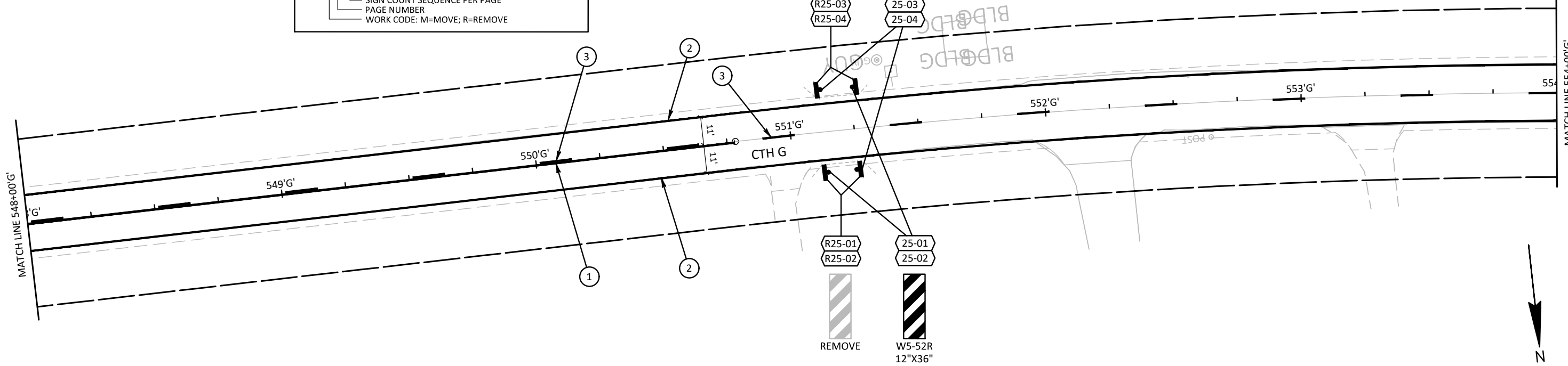
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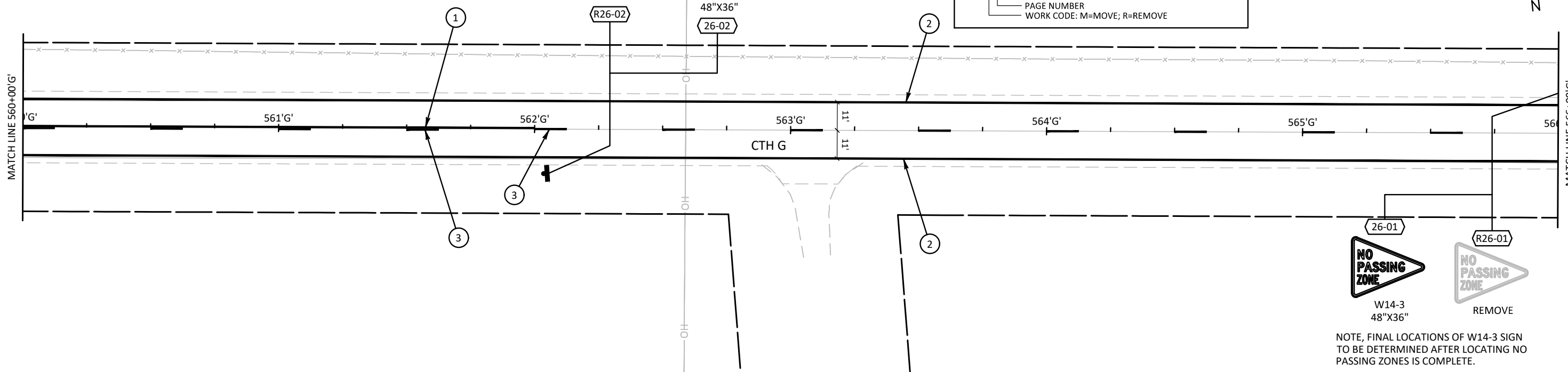
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	PAGE NUMBER
	WORK CODE: M=MOVE; R=REMOVE



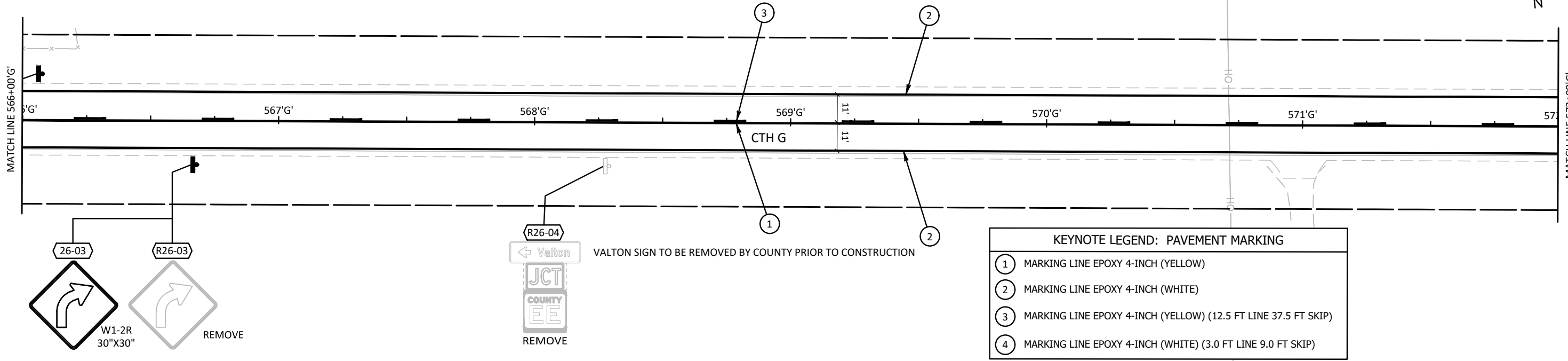
KEYNOTE LEGEND: PAVEMENT MARKING	
①	MARKING LINE EPOXY 4-INCH (YELLOW)
②	MARKING LINE EPOXY 4-INCH (WHITE)
③	MARKING LINE EPOXY 4-INCH (YELLOW) (12.5 FT LINE 37.5 FT SKIP)
④	MARKING LINE EPOXY 4-INCH (WHITE) (3.0 FT LINE 9.0 FT SKIP)

NOTE, FINAL LOCATIONS OF W14-3 SIGN TO BE DETERMINED AFTER LOCATING NO PASSING ZONES IS COMPLETE.

PERMANENT SIGNING LEGEND	
	EXISTING SIGN MOUNTED ON POST(S)
	PROPOSED SIGN MOUNTED ON POST(S)
	DENOTES SIGN NUMBER
	SIGN COUNT SEQUENCE PER PAGE
	PAGE NUMBER
	WORK CODE: M=MOVE; R=REMOVE



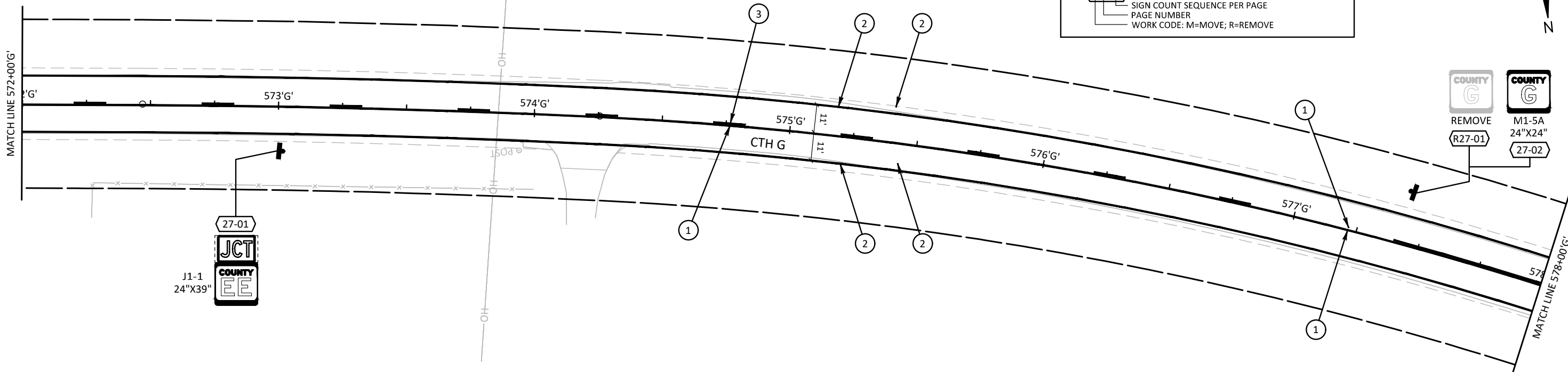
NOTE, FINAL LOCATIONS OF W14-3 SIGN TO BE DETERMINED AFTER LOCATING NO PASSING ZONES IS COMPLETE.



KEYNOTE LEGEND: PAVEMENT MARKING	
①	MARKING LINE EPOXY 4-INCH (YELLOW)
②	MARKING LINE EPOXY 4-INCH (WHITE)
③	MARKING LINE EPOXY 4-INCH (YELLOW) (12.5 FT LINE 37.5 FT SKIP)
④	MARKING LINE EPOXY 4-INCH (WHITE) (3.0 FT LINE 9.0 FT SKIP)

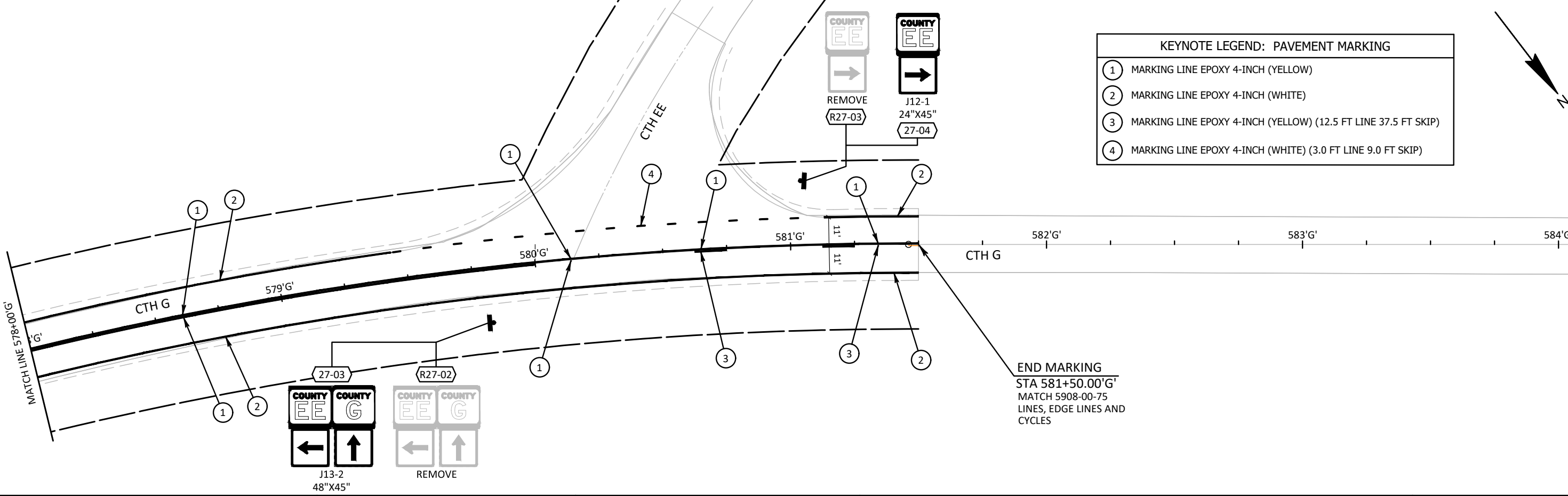
**PERMANENT SIGNING LEGEND**

- EXISTING SIGN MOUNTED ON POST(S)
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- DENOTES SIGN NUMBER
- SIGN COUNT SEQUENCE PER PAGE
- PAGE NUMBER
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**KEYNOTE LEGEND: PAVEMENT MARKING**

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Estimate Of Quantities By Plan Sets

5908-00-74

Line	Item	Item Description	Unit	Total	Qty
0002	204.0115	Removing Asphaltic Surface Butt Joints	SY	3,258.000	3,258.000
0004	204.0120	Removing Asphaltic Surface Milling	SY	2,176.000	2,176.000
0006	211.0700.S	Prepare Foundation for CIR Base Layer (project) 01. 5908-00-74	EACH	1.000	1.000
0010	211.0800.S	Base Repair for CIR Layer	CY	518.000	518.000
0012	213.0100	Finishing Roadway (project) 01. 5908-00-74	EACH	1.000	1.000
0016	305.0110	Base Aggregate Dense 3/4-Inch	TON	4,914.000	4,914.000
0018	327.1000.S	CIR Asphaltic Base Layer	SY	77,675.000	77,675.000
0020	455.0605	Tack Coat	GAL	9,857.000	9,857.000
0022	455.0770.S	Asphalt Stabilizing Agent	TON	348.000	348.000
0024	460.0105.S	HMA Percent Within Limits (PWL) Test Strip Volumetrics	EACH	1.000	1.000
0026	460.0110.S	HMA Percent Within Limits (PWL) Test Strip Density	EACH	2.000	2.000
0028	460.2000	Incentive Density HMA Pavement	DOL	340.000	340.000
0030	460.2005	Incentive Density PWL HMA Pavement	DOL	15,510.000	15,510.000
0032	460.2007	Incentive Density HMA Pavement Longitudinal Joints	DOL	63,858.000	63,858.000
0034	460.2010	Incentive Air Voids HMA Pavement	DOL	15,820.000	15,820.000
0036	460.6223	HMA Pavement 3 MT 58-28 S	TON	518.000	518.000
0038	460.6224	HMA Pavement 4 MT 58-28 S	TON	15,817.000	15,817.000
0040	465.0120	Asphaltic Surface Driveways and Field Entrances	TON	75.000	75.000
0042	614.0400	Adjusting Steel Plate Beam Guard	LF	187.500	187.500
0044	618.0100	Maintenance And Repair of Haul Roads (project) 01. 5908-00-74	EACH	1.000	1.000
0048	619.1000	Mobilization	EACH	0.500	0.500
0050	624.0100	Water	MGAL	221.000	221.000
0052	634.0612	Posts Wood 4x6-Inch X 12-FT	EACH	4.000	4.000
0054	634.0614	Posts Wood 4x6-Inch X 14-FT	EACH	106.000	106.000
0056	634.0616	Posts Wood 4x6-Inch X 16-FT	EACH	18.000	18.000
0058	634.0618	Posts Wood 4x6-Inch X 18-FT	EACH	29.000	29.000
0060	637.2210	Signs Type II Reflective H	SF	72.090	72.090
0062	637.2230	Signs Type II Reflective F	SF	918.730	918.730
0064	638.2602	Removing Signs Type II	EACH	66.000	66.000
0066	638.3000	Removing Small Sign Supports	EACH	47.000	47.000
0068	642.5201	Field Office Type C	EACH	0.500	0.500
0070	643.0420	Traffic Control Barricades Type III	DAY	1,600.000	1,600.000
0072	643.0705	Traffic Control Warning Lights Type A	DAY	3,200.000	3,200.000
0074	643.0900	Traffic Control Signs	DAY	3,250.000	3,250.000
0076	643.0920	Traffic Control Covering Signs Type II	EACH	4.000	4.000
0078	643.1050	Traffic Control Signs PCMS	DAY	7.000	7.000
0080	643.5000	Traffic Control	EACH	0.500	0.500
0082	646.1020	Marking Line Epoxy 4-Inch	LF	122,778.000	122,778.000
0084	648.0100	Locating No-Passing Zones	MI	6.040	6.040
0086	650.8000	Construction Staking Resurfacing Reference	LF	32,009.000	32,009.000
0088	650.9911	Construction Staking Supplemental Control (project) 01.5908-00-74	EACH	1.000	1.000
0092	690.0150	Sawing Asphalt	LF	699.000	699.000
0094	740.0440	Incentive IRI Ride	DOL	23,719.000	23,719.000
0096	ASP.1T0A	On-the-Job Training Apprentice at \$5.00/HR	HRS	475.000	475.000
0098	ASP.1T0G	On-the-Job Training Graduate at \$5.00/HR	HRS	300.000	300.000
0100	SPV.0060	Special 01. Landmark Reference Monuments Special	EACH	1.000	1.000
0102	SPV.0195	Special 01. Base Aggregate Dense 3/4-Inch Beam Guard	TON	9.000	9.000

**REMOVING ASPHALTIC PAVEMENT**

**PREPARE FOUNDATION FOR CIR BASE LAYER**

**DRIVEWAYS AND FIELD ENTRANCES**

STATION	LOCATION	204.0115	204.0120	REMARKS
		REMOVING ASPHALTIC SURFACE BUTT JOINTS	REMOVING ASPHALTIC SURFACE MILLING	
261+41 - 263+41	LT&RT	524	0	START OF PROJECT
274+94 - 276+94	LT&RT	702	0	B-56-221
277+74 - 279+74	LT&RT	630	0	B-56-221
JESSOP RD	RT	53	233	-
BAUER RD	RT	56	302	-
BUNDY HOLOW RD	RT	74	57	-
NASH RD	LT	148	0	-
SLONIKER RD	RT	68	189	-
NORWEGIAN CHURCH RD	LT	72	109	-
QUAKER VALLEY RD	LT	77	443	-
BOOT JACK RD	LT	129	89	-
BULIN RD	RT	79	19	-
BIRDD RD	RT	102	24	-
FARRA RD	RT	147	84	-
N ROTT RD	RT	86	0	-
CTH EE	LT	238	257	-
SUBTOTAL		3185	1804	

LOCATION	EACH	NOTES
PROJECT	1	-
PROJECT TOTALS	1	

STATION	LOCATION	*305.0110	465.0120	REMARKS
		BASE AGGREGATE DENSE 3/4-INCH	ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES	
267+60	RT	-	5	ASPH, PE
267+60	LT	2	-	AGG, FE
272+22	RT	4	-	AGG, PE
274+15	LT	3	-	AGG, PE
319+64	RT	4	-	AGG, PE
323+37	LT	-	3	ASPH, PE
332+52	RT	-	6	ASPH, PE
335+81	RT	-	4	ASPH, PE
338+29	RT	-	6	ASPH, PE
342+45	LT	1	-	AGG, PE
358+79	LT	-	6	ASPH, PE
374+86	RT	2	-	AGG, FE
377+56	RT	-	5	ASPH, PE
382+69	LT	2	-	AGG, PE
387+36	RT	2	-	AGG, FE
388+08	RT	2	-	AGG, PE
390+33	RT	2	-	AGG, FE
390+61	RT	2	-	AGG, FE
399+32	RT	2	-	AGG, PE
403+44	RT	3	-	AGG, PE
414+58	LT	2	-	AGG, PE
415+95	RT	2	-	AGG, PE
437+83	RT	2	-	AGG, PE
451+73	LT	-	11	ASPH, PE
461+62	RT	-	5	ASPH, PE
471+69	RT	-	6	ASPH, PE
471+94	LT	-	2	ASPH, FE
472+87	RT	-	5	ASPH, PE
508+70	LT	2	-	AGG, PE
510+26	LT	2	-	AGG, PE
520+97	RT	2	-	AGG, PE
524+48	RT	4	-	AGG, PE
525+27	LT	5	-	AGG, PE
526+03	LT	3	-	AGG, PE
550+95	RT	1	-	AGG, FE
552+18	RT	-	6	ASPH, PE
553+27	RT	2	-	AGG, PE
563+04	RT	2	-	AGG, PE
570+98	RT	2	-	AGG, FE
574+21	RT	-	4	ASPH, PE
SUBTOTALS		61	75	
PROJECT TOTALS		61	75	

**FINISHING ROADWAY**

LOCATION	EACH	NOTES
PROJECT	1	-
PROJECT TOTALS	1	

**BASES AND SUBBASES**

267+60	RT	5	27	ASPH, PE
323+37	LT	3	14	ASPH, PE
332+52	RT	5	28	ASPH, PE
335+81	RT	4	23	ASPH, PE
338+29	RT	5	32	ASPH, PE
358+79	LT	5	29	ASPH, PE
377+56	RT	6	26	ASPH, PE
451+73	LT	12	52	ASPH, PE
461+62	RT	5	27	ASPH, PE
471+69	RT	6	28	ASPH, PE
471+94	LT	3	11	ASPH, FE
472+87	RT	5	22	ASPH, PE
552+18	RT	6	30	ASPH, PE
574+21	RT	4	22	ASPH, PE
SUBTOTAL		73	372	
PROJECT TOTALS		3258	2176	

STATION - STATION	LOCATION	TON	TON	REMARKS
261+41 - 276+94	LT&RT	220	7	
277+40 - 372+52	LT&RT	1431	2	
372+52 - 496+17	LT&RT	1889	-	
496+17 - 581+50	LT&RT	1287	-	
SUBTOTAL		4827	9	
STATE LANE	RT	4	-	AGG SIDEROAD
S ROTT RD	LT	3	-	AGG SIDEROAD
CTH EE	LT	19	-	
SUBTOTAL		26	0	
PROJECT TOTALS		4853	9	

\*ADDITIONAL QUANTITIES SHOWN ELSEWHERE ON PLANS

\*ADDITIONAL QUANTITIES SHOWN ELSEWHERE ON PLANS

\*BID ITEMS ARE UNDER CATEGORY 0010, UNLESS OTHERWISE NOTED



**ASPHALTIC ITEMS SUMMARY**

STATION	LOCATION	211.0800.S	327.1000.S	455.0770.S			REMARKS	
		BASE REPAIR	COLD IN-PLACE	455.0605	ASPHALT	460.6223		460.6224
		FOR CIR	RECYCLING	TACK	STABILIZING	HMA PAVEMENT		HMA PAVEMENT
		LAYER	(CIR) ASPHALT BASE LAYER	COAT	AGENT	3 MT 58-28 S	4 MT 58-28 S	
		CY	SY	GAL	TON	TON	TON	
261+41 - 276+94	ML	26	3828	544	17	-	889	-
277+40 - 372+52	ML	152	22763	2807	102	-	4587	-
372+52 - 496+17	ML	202	30226	3627	135	-	5935	-
496+17 - 581+50	ML	139	20858	2503	93	-	4099	-
SUBTOTAL		518	77675	9481	348	0	15510	
JESSOP RD	RT	-	-	34	-	55	28	-
BAUER RD	RT	-	-	43	-	69	35	-
BUNDY HOLLOW RD	RT	-	-	16	-	20	13	-
NASH RD	LT	-	-	18	-	16	15	-
SLONIKER RD	RT	-	-	31	-	47	25	-
NORWEGIAN CHURCH RD	LT	-	-	22	-	30	18	-
QUAKER VALLEY RD	LT	-	-	62	-	101	51	-
BOOT JACK RD	LT	-	-	26	-	32	21	-
BULIN RD	RT	-	-	12	-	12	10	-
BIRDD RD	RT	-	-	15	-	16	12	-
FARRA RD	RT	-	-	28	-	33	23	-
N ROTT RD	RT	-	-	10	-	9	8	-
CTH EE	LT	-	-	59	-	79	48	-
INTERSECTIONS		0	0	376	0	518	307	
<b>PROJECT TOTAL</b>		<b>518</b>	<b>77675</b>	<b>9857</b>	<b>348</b>	<b>518</b>	<b>15817</b>	

**HMA PERCENT WITHIN LIMITS (PWL)**

LOCATION	460.0105.S	460.0100.S
	TEST	TEST
	STRIP	STRIP
	VOLUMETRICS	DENSITY
PROJECT	EACH	EACH
	1	2
<b>PROJECT TOTALS</b>	<b>1</b>	<b>2</b>

**ADJUSTING STEEL PLATE BEAM GUARD**

STATION - STATION	LOCATION	LF	REMARKS
275+26 - 276+95	LT	125.0	
277+74 - 278+47	LT	62.5	
<b>PROJECT TOTALS</b>		<b>187.5</b>	

**MAINTENANCE AND REPAIR OF HAUL ROADS**

LOCATION	CATEGORY	EACH
PROJECT	0020	1
<b>PROJECT TOTALS</b>		<b>1</b>

**PWL MIXTURE USE TABLE**

LOCATION	STATION - STATION	MIXTURE USE	UNDERLYING SURFACE	BID ITEM	TONS	THICKNESS	MIXTURE ACCEPTANCE	DENSITY ACCEPTANCE
22-FOOT DRIVING LANE	261+41 - 581+50	LOWER LAYER	CIR PAVEMENT	4 MT 58-28 S	7755	1.75"	PWL INCENTIVE AIR VOIDS HMA PAVEMENT 460.2010	INCENTIVE DENSITY PWL HMA PAVEMENT 460.2005
22-FOOT DRIVING LANE	261+41 - 581+50	UPPER LAYER	4 MT 58-28 S	4 MT 58-28 S	7755	1.75"	PWL INCENTIVE AIR VOIDS HMA PAVEMENT 460.2010	INCENTIVE DENSITY PWL HMA PAVEMENT 460.2005
INTERSECTIONS	261+41 - 581+50	LOWER LAYER	MILLED EXISTING HMA SURFACE	3 MT 58-28 S	518	3.75"	QMP as per SS 460.	Incentive Density HMA Pavement 460.2000
INTERSECTIONS	261+41 - 581+50	UPPER LAYER	3 MT 58-28 S	4 MT 58-28 S	307	1.75"	PWL INCENTIVE AIR VOIDS HMA PAVEMENT 460.2010	ACCEPTANCE TESTING BY THE DEPARTMENT; NOT ELIGIBLE FOR INCENTIVE

**MOBILIZATION**

LOCATION	EACH	NOTES
PROJECT	0.5	PRORATED BETWEEN 5908-00-75
<b>PROJECT TOTALS</b>		<b>0.5</b>

\*BID ITEMS ARE UNDER CATEGORY 0010, UNLESS OTHERWISE NOTED



3

**WATER**

STATION	LOCATION	624.0100 MGAL	REMARKS
261+41 - 276+94	ML	11	CIR AND SHOULDER
277+40 - 372+52	ML	65	CIR AND SHOULDER
372+52 - 496+17	ML	86	CIR AND SHOULDER
496+17 - 581+50	ML	59	CIR AND SHOULDER
SUBTOTAL		221	
<b>PROJECT TOTAL</b>		<b>221</b>	

**PERMANENT SIGNING**

SIGN NO.	LT/RT	SIGN CODE	SIGN MESSAGE	SIZE W" X H"	REFLECTIVE H (SF)	REFLECTIVE F (SF)	637.2210	637.2230	634.0612	634.0614	634.0616	634.0618
							SIGNS TYPE II	SIGNS TYPE II	POSTS WOOD 4x6-INCH 12-FT (EACH)	POSTS WOOD 4x6-INCH 14-FT (EACH)	POSTS WOOD 4X6-INCH 16-FT (EACH)	POSTS WOOD 4X6-INCH 18-FT (EACH)
01-01	RT	M1-5A	CTH G	24"x24"	4.00	-	-	-	-	-	1	-
01-02	RT	R2-1	SPEED LIMIT 55	24"x30"	5.00	-	-	-	-	-	1	-
01-03	RT	W1-4R	CURVES IN ROADWAY	30"x30"	-	6.25	-	-	-	-	-	1
01-04	RT	W13-1	30 MPH	18"x18"	-	2.25	-	-	-	-	-	-
01-05	RT	W11-14	HORSE AND BUGGY	36"x36"	-	9.00	-	-	-	-	-	1
01-06	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	1	-	-	-
01-07	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	1	-	-	-
01-08	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	1	-	-	-
01-09	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-	-	-
01-10	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-	-	-
01-11	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-	-	-
01-12	LT	W3-1	STOP SIGN AHEAD	36"x36"	-	9.00	-	-	-	-	-	1
01-13	LT	J1-1	JCT STH 58	24"x39"	6.50	-	-	-	-	-	1	-
02-01	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	1	-	-	-
02-02	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	1	-	-	-
02-03	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	1	-	-	-
02-04	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	1	-	-	-
02-05	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	1	-	-	-
02-06	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-	-	-
02-07	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-	-	-
02-08	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-	-	-
02-09	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-	-	-
02-10	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-	-	-
02-11	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	1	-	-	-
02-12	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	1	-	-	-
02-13	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	1	-	-	-
02-14	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	1	-	-	-
02-15	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	1	-	-	-
02-16	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	1	-	-	-
02-17	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-	-	-
02-18	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-	-	-
02-19	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-	-	-
02-20	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-	-	-
02-21	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-	-	-
02-22	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-	-	-
03-01	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	1	-	-	-
03-02	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	1	-	-	-
03-03	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	1	-	-	-
03-04	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	1	-	-	-
03-05	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	1	-	-	-
03-06	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-	-	-
03-07	RT	W1-2L	ROADWAY CURVES LEFT	30"x30"	-	6.25	-	-	-	-	-	1
03-08	RT	W13-1	40 MPH	18"x18"	-	2.25	-	-	-	-	-	-
03-09	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-	-	-
03-10	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-	-	-
03-11	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-	-	-
03-12	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-	-	-
03-13	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	1	-	-	-
03-14	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	1	-	-	-
03-15	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-	-	-
03-16	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	1	-	-	-
03-17	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	1	-	-	-
03-18	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-	-	-
03-19	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-	-	-
03-20	LT	W1-4R	CURVES IN ROADWAY	30"x30"	-	6.25	-	-	-	-	-	1
03-21	LT	W13-1	30 MPH	18"x18"	-	2.25	-	-	-	-	-	-
03-22	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-	-	-

CONTINUED

\*BID ITEMS ARE UNDER CATEGORY 0010, UNLESS OTHERWISE NOTED

PROJECT NO: 5908-00-74

HWY: CTH G

COUNTY: SAUK

MISCELLANEOUS QUANTITIES

SHEET

E

3



PERMANENT SIGNING (CON'T)

PERMANENT SIGNING (CON'T)

637.2210 637.2230 634.0612 634.0614 634.0616 634.0618  
 SIGNS SIGNS POSTS POSTS POSTS POSTS  
 TYPE II TYPE II WOOD WOOD WOOD WOOD  
 4x6-INCH 4x6-INCH 4X6-INCH 4X6-INCH

637.2210 637.2230 634.0612 634.0614 634.0616 634.0618  
 SIGNS SIGNS POSTS POSTS POSTS POSTS  
 TYPE II TYPE II WOOD WOOD WOOD WOOD  
 4x6-INCH 4x6-INCH 4X6-INCH 4X6-INCH

	SIGN	SIGN	SIZE	REFLECTIVE H	REFLECTIVE F	12-FT	14-FT	16-FT	18-FT
04-01	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
04-02	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
04-03	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
04-04	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
04-05	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
04-06	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
04-07	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
04-08	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
04-09	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
04-10	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
04-11	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
04-12	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
04-13	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
04-14	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
04-15	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
04-16	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
04-17	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
04-18	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
05-01	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
05-02	RT	W1-2R	ROADWAY CURVES RIGHT	30"x30"	-	6.25	-	-	1
05-03	RT	W13-1	40 MPH	18"x18"	-	2.25	-	-	-
05-04	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
05-05	RT	W1-4R	CURVES IN ROADWAY	30"x30"	-	6.25	-	-	1
05-06	RT	W13-1	40 MPH	18"x18"	-	2.25	-	-	-
06-01	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
06-02	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
06-03	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
06-04	RT	DR1-53R	RUSTIC ROAD RIGHT	30"x8"	1.67	-	-	1	-
06-05	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
06-06	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
06-07	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
06-08	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
06-09	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
06-10	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
06-11	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
06-12	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
06-13	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
06-14	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
06-15	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
06-16	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
06-17	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
06-18	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
06-19	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
06-20	LT	DR1-53L	RUSTIC ROAD LEFT	30"x8"	1.67	-	-	1	-
07-01	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
07-02	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
07-03	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
07-04	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
07-05	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
07-06	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
07-07	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
07-08	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
07-09	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
07-10	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
07-11	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
07-12	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
07-13	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
07-14	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
07-15	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
07-16	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
07-17	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
07-18	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
07-19	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
07-20	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-

	SIGN	SIGN	SIZE	REFLECTIVE H	REFLECTIVE F	12-FT	14-FT	16-FT	18-FT
08-01	LT	W14-3	NO PASSING ZONE	48"x36"	-	5.56	-	-	1
08-02	RT	W14-3	NO PASSING ZONE	48"x36"	-	5.56	-	-	1
08-03	LT	W1-4R	CURVES IN ROADWAY	30"x30"	-	6.25	-	-	1
08-04	LT	W13-1	40 MPH	18"x18"	-	2.25	-	-	-
09-01	LT	W14-3	NO PASSING ZONE	48"x36"	-	5.56	-	-	1
09-02	RT	W1-4L	CURVES IN ROADWAY	30"x30"	-	6.25	-	-	1
09-03	RT	W13-1	35 MPH	18"x18"	-	2.25	-	-	-
09-04	RT	W14-3	NO PASSING ZONE	48"x36"	-	5.56	-	-	1
10-01	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
10-02	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
10-03	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
10-04	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
10-05	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
10-06	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
10-07	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
10-08	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
10-09	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
10-10	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
10-11	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
10-12	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
10-13	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
10-14	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
10-15	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
10-16	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
10-17	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
10-18	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
11-01	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
11-02	LT	W1-4L	CURVES IN ROADWAY	30"x30"	-	6.25	-	-	1
11-03	LT	W13-1	35 MPH	18"x18"	-	2.25	-	-	-
11-04	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
12-01	RT	S1-1	SCHOOL ZONE	36"x36"	6.75	-	-	-	1
12-02	RT	W1-3R	TURNS IN ROADWAY	36"x36"	-	9.00	-	-	1
12-03	RT	W13-1	30 MPH	18"x18"	-	2.25	-	-	-
12-04	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
12-05	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
12-06	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
12-07	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
13-01	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
13-02	LT	S1-1	SCHOOL ZONE	36"x36"	6.75	-	-	-	1
13-03	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
13-04	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
13-05	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
13-06	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
13-07	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
13-08	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
13-09	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
13-10	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
13-11	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
13-12	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
13-13	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
13-14	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
13-15	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
13-16	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
13-17	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
13-18	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
13-19	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
13-20	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
13-21	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
13-22	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
13-23	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
13-24	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
13-25	LT	S1-1	SCHOOL ZONE	36"x36"	6.75	-	-	-	1

CONTINUED

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PERMANENT SIGNING (CON'T)

PERMANENT SIGNING (CON'T)

637.2210 637.2230 634.0612 634.0614 634.0616 634.0618  
 SIGNS SIGNS POSTS POSTS POSTS POSTS  
 TYPE II TYPE II WOOD WOOD WOOD WOOD  
 4x6-INCH 4x6-INCH 4X6-INCH 4X6-INCH

637.2210 637.2230 634.0612 634.0614 634.0616 634.0618  
 SIGNS SIGNS POSTS POSTS POSTS POSTS  
 TYPE II TYPE II WOOD WOOD WOOD WOOD  
 4x6-INCH 4x6-INCH 4X6-INCH 4X6-INCH

	SIGN	SIGN	SIZE	REFLECTIVE H	REFLECTIVE E	12-FT	14-FT	16-FT	18-FT
13-26	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
13-27	LT	W2-2	THREE WAY INTERSECTION	30"x30"	-	6.25	-	-	1
14-01	LT	W1-3R	TURNS IN ROADWAY	36"x36"	-	9.00	-	-	1
14-02	LT	W13-1	30 MPH	18"x18"	-	2.25	-	-	-
15-01	LT	W2-2	THREE WAY INTERSECTION	30"x30"	-	6.25	-	-	1
16-01	RT	W1-4L	CURVES IN ROADWAY	30"x30"	-	6.25	-	-	1
16-02	RT	W13-1	30 MPH	18"x18"	-	2.25	-	-	-
17-01	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
17-02	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
17-03	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
17-04	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
17-05	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
17-06	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
17-07	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
17-08	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
17-09	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
17-10	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
17-11	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
17-12	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
17-13	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
17-14	RT	W11-4	CATTLE CROSSING	30"x30"	-	6.25	-	-	1
17-15	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
17-16	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
17-17	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
18-01	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
18-02	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
18-03	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
18-04	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
18-05	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
18-06	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
18-07	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
18-08	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
18-09	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
18-10	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
18-11	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
18-12	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
18-13	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
18-14	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
19-01	LT	W11-4	CATTLE CROSSING	30"x30"	-	6.25	-	-	1
20-01	RT	W1-2L	ROADWAY CURVES LEFT	30"x30"	-	6.25	-	-	1
20-02	RT	W13-1	35 MPH	18"x18"	-	2.25	-	-	-
20-03	LT	W1-4L	CURVES IN ROADWAY	30"x30"	-	6.25	-	-	1
20-04	LT	W13-1	30 MPH	18"x18"	-	2.25	-	-	-
20-05	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
20-06	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
20-07	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
20-08	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
20-09	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
20-10	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
20-11	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
20-12	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
20-13	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
20-14	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
21-01	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-
21-02	RT	W1-2R	ROADWAY CURVES RIGHT	30"x30"	-	6.25	-	-	1
21-03	LT	W1-2R	ROADWAY CURVES RIGHT	30"x30"	-	6.25	-	-	1
21-04	LT	W13-1	35 MPH	18"x18"	-	2.25	-	-	-
21-05	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-
22-01	RT	W11-2	PEDESTRIAN	30"x30"	-	6.25	-	-	1
22-02	RT	W13-1	40 MPH	18"x18"	-	2.25	-	-	-
22-03	LT	W1-2L	ROADWAY CURVES LEFT	30"x30"	-	6.25	-	-	1

	SIGN	SIGN	SIZE	REFLECTIVE H	REFLECTIVE E	12-FT	14-FT	16-FT	18-FT		
23-01	RT	W1-2L	ROADWAY CURVES LEFT	30"x30"	-	6.25	-	-	1		
23-02	RT	W13-1	45 MPH	18"x18"	-	2.25	-	-	-		
23-03	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-		
23-04	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-		
23-05	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-		
23-06	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-		
23-07	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-		
23-08	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-		
23-09	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-		
23-10	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-		
23-11	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-		
23-12	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-		
23-13	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-		
23-14	LT	W11-2	PEDESTRIAN	30"x30"	-	6.25	-	-	1		
23-15	LT	W13-1	40 MPH	18"x18"	-	2.25	-	-	-		
23-16	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-		
23-17	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-		
23-18	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-		
24-01	LT	W1-2R	ROADWAY CURVES RIGHT	30"x30"	-	6.25	-	-	1		
24-02	LT	W13-1	45 MPH	18"x18"	-	2.25	-	-	-		
24-03	RT	W14-3	NO PASSING ZONE	48"x36"	-	5.56	-	-	1		
24-04	RT	W5-2	NARROW BRIDGE	36"x36"	-	9.00	-	-	1		
24-05	LT	W14-3	NO PASSING ZONE	48"x36"	-	5.56	-	-	1		
25-01	RT	W5-52R	BRIDGE HAZARD	12"x36"	-	3.00	1	-	-		
25-02	LT	W5-52R	BRIDGE HAZARD	12"x36"	-	3.00	1	-	-		
25-03	LT	W5-52L	BRIDGE HAZARD	12"x36"	-	3.00	1	-	-		
25-04	RT	W5-52L	BRIDGE HAZARD	12"x36"	-	3.00	1	-	-		
25-05	LT	W5-2	NARROW BRIDGE	36"x36"	-	9.00	-	-	1		
26-01	LT	W14-3	NO PASSING ZONE	48"x36"	-	5.56	-	-	1		
26-02	RT	W14-3	NO PASSING ZONE	48"x36"	-	5.56	-	-	1		
26-03	RT	W1-2R	ROADWAY CURVES RIGHT	30"x30"	-	6.25	-	-	1		
27-01	RT	J1-1	JCT CTH EE	24"x39"	6.50	-	-	-	1		
27-02	LT	M1-5A	CTH G	24"x24"	4.00	-	-	-	1		
27-03	RT	J13-2	CTH EE LEFT, CTH G THRU	48"x45"	15.00	-	-	-	1		
27-04	LT	J12-1	CTH EE LEFT	24"x45"	7.50	-	-	-	1		
PROJECT TOTALS						72.09	918.73	4.00	106.00	18.00	29.00

CONTINUED

\*BID ITEMS ARE UNDER CATEGORY 0010, UNLESS OTHERWISE NOTED

**REMOVING SIGNS**

SIGN #	SIGN MESSAGE	LOCATION	638.3000	
			638.2602 REMOVING SIGNS TYPE II (EACH)	REMOVING SMALL SIGN SUPPORTS (EACH)
R01-01	CTH G	CTH G	1	1
R01-02	SPEED LIMIT 55 MPH	CTH G	1	1
R01-03	HORSE AND BUGGY	CTH G	1	1
R01-04	NEXT 10 MILES	CTH G	1	-
R01-05	STOP SIGN AHEAD	CTH G	1	1
R01-06	JCT WIS 58	CTH G	1	1
R03-01	ROADWAY CURVES LEFT	CTH G	1	1
R03-02	CURVES IN ROADWAY	CTH G	1	1
R03-03	40 MPH	CTH G	1	-
R05-01	CURVES IN ROADWAY	CTH G	1	1
R05-02	35 MPH	CTH G	1	-
R05-03	ROADWAY CURVES RIGHT	CTH G	1	1
R05-04	35 MPH	CTH G	1	-
R06-01	RUSTIC ROAD RIGHT	CTH G	1	1
R06-02	RUSTIC ROAD LEFT	CTH G	1	1
R08-01	NO PASSING ZONE	CTH G	1	1
R08-02	NO PASSING ZONE	CTH G	1	1
R08-03	CURVES IN ROADWAY	CTH G	1	1
R08-04	35 MPH	CTH G	1	-
R09-01	CURVES IN ROADWAY	CTH G	1	1
R09-02	40 MPH	CTH G	1	-
R09-03	NO PASSING ZONE	CTH G	1	1
R11-01	CURVES IN ROADWAY	CTH G	1	1
R11-02	40 MPH	CTH G	1	-
R12-01	TURNS IN ROADWAY AHEAD	CTH G	1	1
R12-02	25 MPH	CTH G	1	-
R12-03	SCHOOL ZONE	CTH G	1	1
R13-01	SCHOOL ZONE	CTH G	1	1
R13-02	SCHOOL ZONE	CTH G	1	1
R13-03	THREE WAY INTERSECTION	CTH G	1	1
R14-01	CURVES IN ROADWAY	CTH G	1	1
R14-02	25 MPH	CTH G	1	-
R15-01	THREE WAY INTERSECTION	CTH G	1	1
R16-01	CURVES IN ROADWAY	CTH G	1	1
R16-02	45 MPH	CTH G	1	-
R17-01	CATTLE CROSSING	CTH G	1	1
R17-02	CATTLE XING	CTH G	1	-
R19-01	CATTLE CROSSING	CTH G	1	1
R19-02	CATTLE XING	CTH G	1	-
R20-01	ROADWAY CURVES LEFT	CTH G	1	1
R20-02	35 MPH	CTH G	1	-
R20-03	CURVES IN ROADWAY	CTH G	1	1
R20-04	45 MPH	CTH G	1	-

CONTINUED

SIGN #	SIGN MESSAGE	LOCATION	638.3000	
			638.2602 REMOVING SIGNS TYPE II (EACH)	REMOVING SMALL SIGN SUPPORTS (EACH)
R21-01	CURVES IN ROADWAY	CTH G	1	1
R21-02	45 MPH	CTH G	1	-
R21-03	ROADWAY CURVES RIGHT	CTH G	1	1
R21-04	35 MPH	CTH G	1	-
R22-01	PEDESTRIAN	CTH G	1	1
R22-02	40 MPH	CTH G	1	-
R23-01	PEDESTRIAN	CTH G	1	1
R23-02	40 MPH	CTH G	1	-
R24-01	CURVES IN ROADWAY / 45 MPH	CTH G	1	1
R24-02	45 MPH	CTH G	1	-
R24-03	NO PASSING ZONE	CTH G	1	1
R24-04	NO PASSING ZONE	CTH G	1	1
R25-01	BRIDGE HAZARD	CTH G	1	1
R25-02	BRIDGE HAZARD	CTH G	1	1
R25-03	BRIDGE HAZARD	CTH G	1	1
R25-04	BRIDGE HAZARD	CTH G	1	1
R26-01	NO PASSING ZONE	CTH G	1	1
R26-02	NO PASSING ZONE	CTH G	1	1
R26-03	ROADWAY CURVES RIGHT	CTH G	1	1
R26-04	JCT CTH EE	CTH G	1	1
R27-01	CTH G	CTH G	1	1
R27-02	CTH EE LEFT / CTH G THRU	CTH G	1	1
R27-03	CTH EE RIGHT	CTH G	1	1
<b>PROJECT TOTALS</b>			<b>66</b>	<b>47</b>

FIELD OFFICE TYPE C

LOCATION	EACH	NOTES
PROJECT	0.5	PRORATED BETWEEN 5908-00-75
<b>PROJECT TOTALS</b>	<b>0.5</b>	

\*BID ITEMS ARE UNDER CATEGORY 0010, UNLESS OTHERWISE NOTED

3

3

**TRAFFIC CONTROL**

LOCATION	DAYS	643.0420		643.0705		643.0900		**643.0920		643.1050		NOTES
		TRAFFIC CONTROL	BARRICADES	WARNING LIGHTS	TYPE III	TYPE A	TRAFFIC CONTROL	SIGNS	COVERING SIGNS	TYPE II	TRAFFIC CONTROL	
PRE-WARNING	7	0	0	0	0	0	0	0	0	1	7	PRE-WARN PRIOR TO CONSTRUCTION START
ENTIRE PROJECT	50	32	1600	64	3200	65	3250	4	0	0		
SUBTOTAL			1600		3200		3250	4		7		
<b>PROJECT TOTALS</b>			<b>1600</b>		<b>3200</b>		<b>3250</b>	<b>4</b>		<b>7</b>		

\*\*NUMBER OF CYCLES = 1

**SAWING ASPHALT**

STATION	LOCATION	LF	REMARKS
261+41	LT&RT	33	START OF PROJECT
JESSOP RD	RT	19	-
BAUER RD	RT	20	-
BUNDY HOLOW RD	RT	21	-
NASH RD	LT	51	-
SLONIKER RD	RT	21	-
NORWEGIAN CHURCH RD	LT	22	-
QUAKER VALLEY RD	LT	24	-
BOOT JACK RD	LT	30	-
BULIN RD	RT	20	-
BIRDD RD	RT	24	-
FARRA RD	RT	48	-
N ROTT RD	RT	23	-
CTH EE	LT	24	-
		380	
267+60	RT	21	ASPH, PE
323+37	LT	12	ASPH, PE
332+52	RT	22	ASPH, PE
335+81	RT	16	ASPH, PE
338+29	RT	23	ASPH, PE
358+79	LT	23	ASPH, PE
377+56	RT	26	ASPH, PE
451+73	LT	52	ASPH, PE
461+62	RT	20	ASPH, PE
471+69	RT	26	ASPH, PE
471+94	LT	11	ASPH, FE
472+87	RT	23	ASPH, PE
552+18	RT	26	ASPH, PE
574+21	RT	18	ASPH, PE
		319	
<b>PROJECT TOTALS</b>		<b>699</b>	

**MARKING LINE ITEMS**

LOCATION	EACH	NOTES	STATION - STATION	LOCATION	646.1020			
					MARKING LINE EPOXY 4-INCH			
					SOLID	SOLID	3' LINE 9'SKIP	12.5' LINE 37.5'SKIP
					WHITE	YELLOW	WHITE	YELLOW
					LF	LF	LF	LF
	643.5000		261+41 - 277+40	LT&RT	3,198	3,198	-	-
PROJECT	0.5	PRORATED BETWEEN 5908-00-75	277+40 - 372+52	LT&RT	18,410	18,953	78	663
			372+52 - 496+17	LT&RT	24,129	24,730	48	-
			496+17 - 581+50	LT&RT	16,631	11,484	81	1,175
<b>PROJECT TOTALS</b>	<b>0.5</b>		SUBTOTAL		62,368	58,365	207	1,838
			<b>PROJECT TOTAL</b>				<b>122,778</b>	

**TRAFFIC CONTROL**

**LOCATING NO-PASSING ZONES**

LOCATION	MI	NOTES
PROJECT	6.04	-
<b>PROJECT TOTALS</b>	<b>6.04</b>	

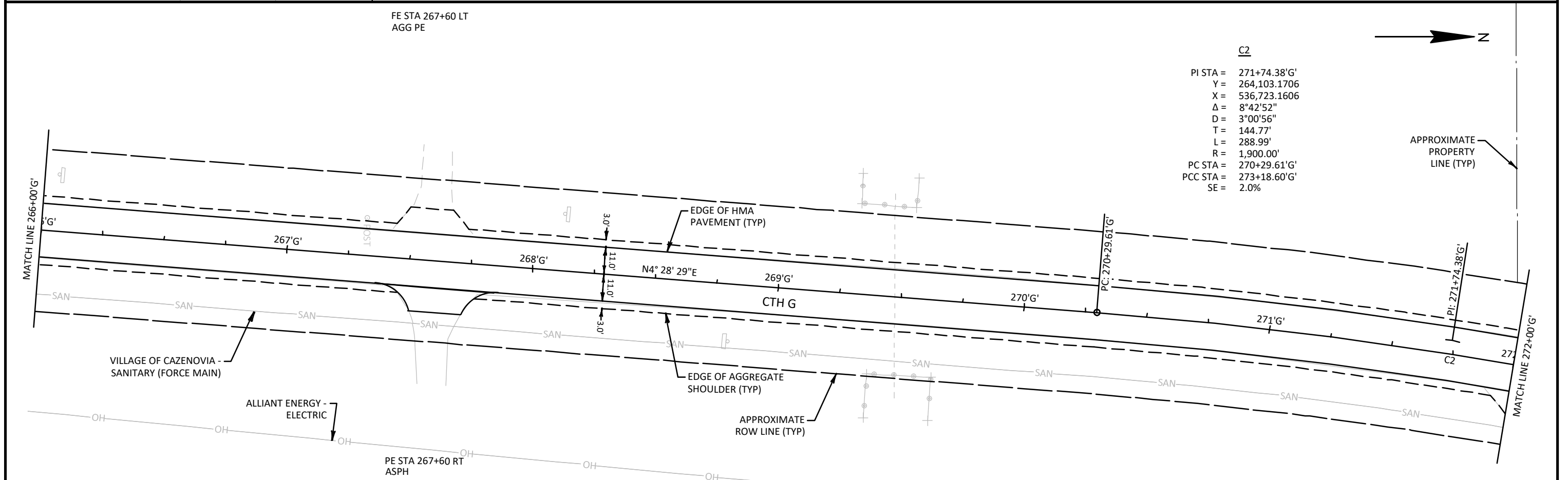
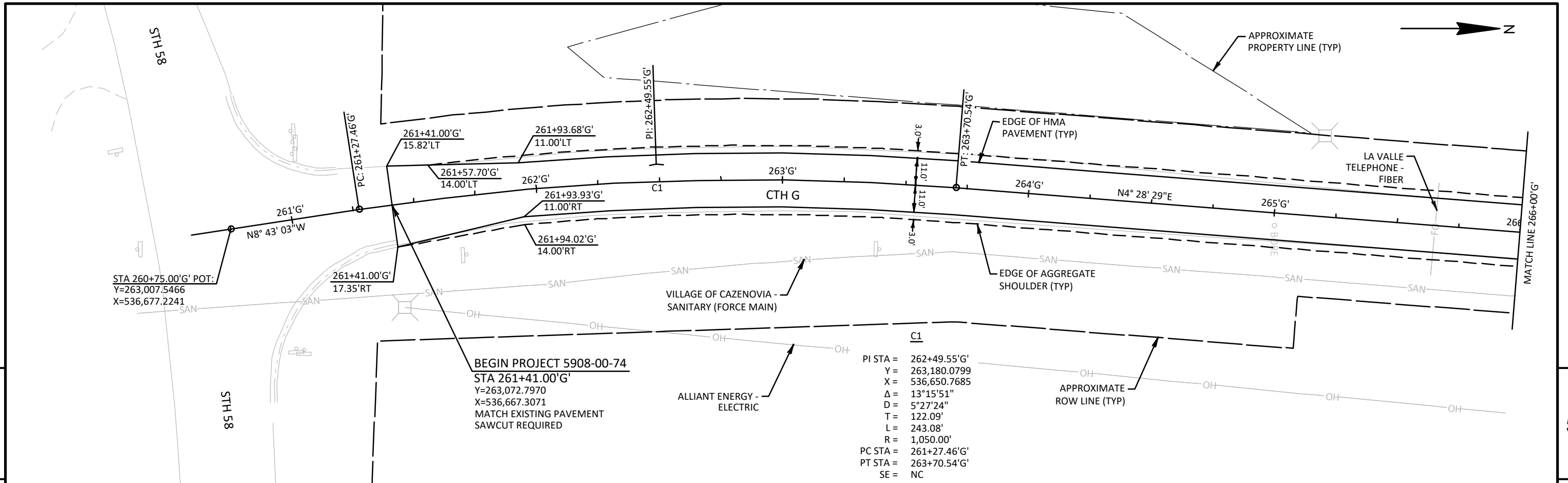
**CONSTRUCTION STAKING**

STATION	LF	LS	NOTES
261+41 - 581+50	32009	-	-
PROJECT	-	1	-
<b>PROJECT TOTALS</b>	<b>32009</b>	<b>1</b>	

**LANDMARK REFERENCE MONUMENTS SPECIAL**

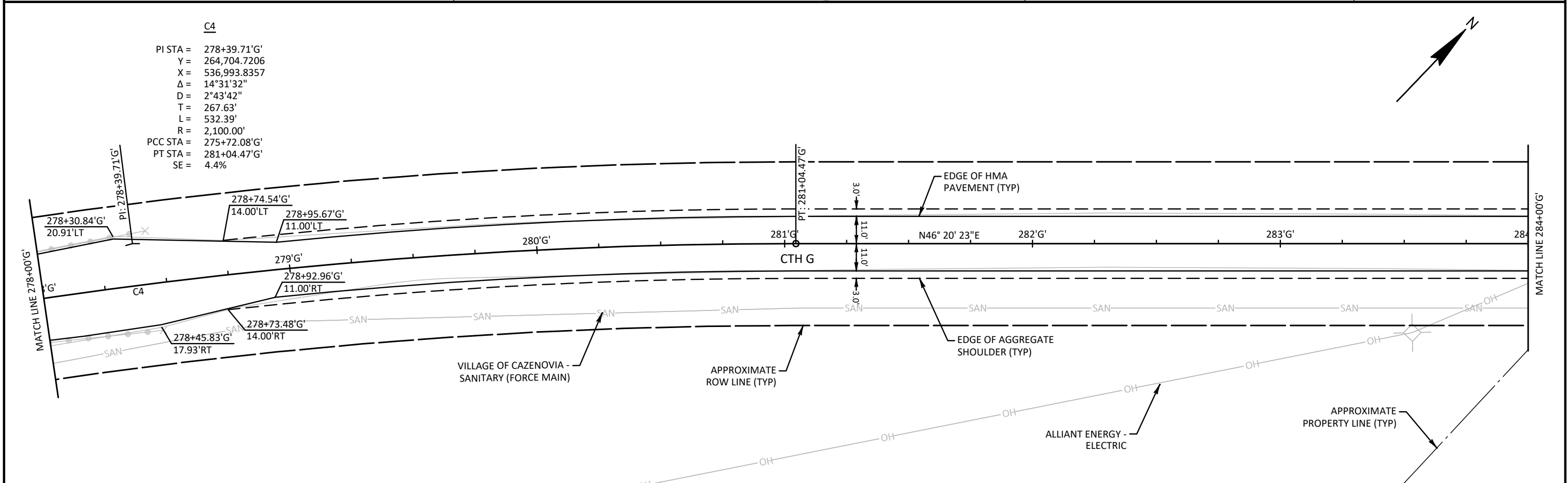
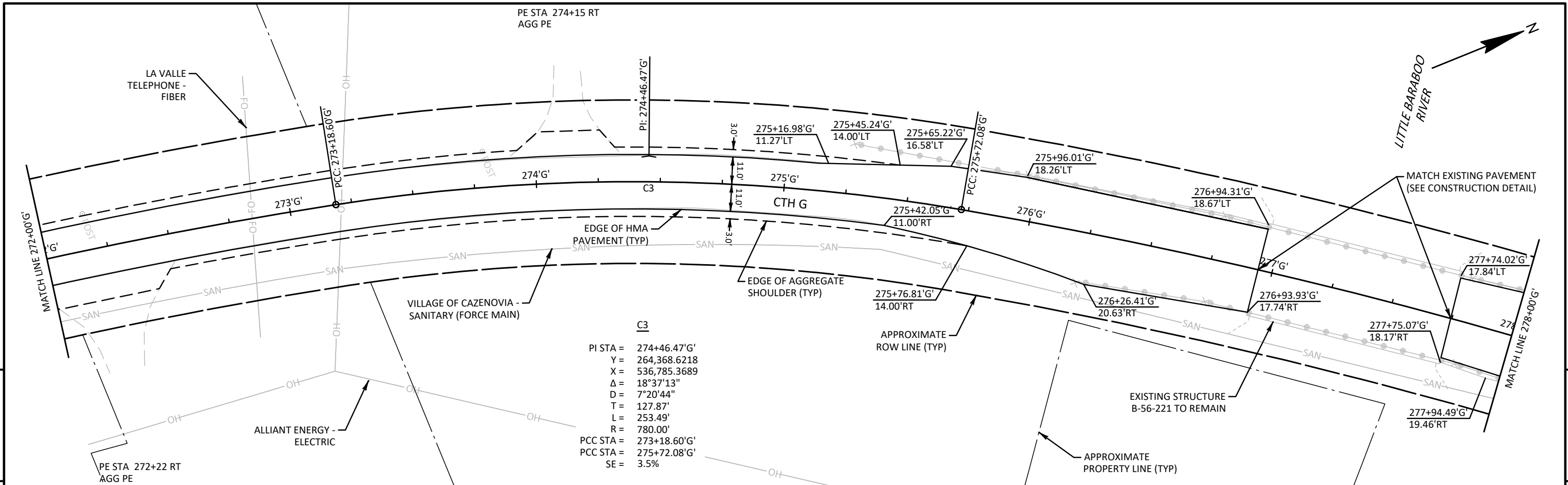
LOCATION	EACH
503+83.8, 1.8' RT	1
<b>PROJECT TOTALS</b>	<b>1</b>

\*BID ITEMS ARE UNDER CATEGORY 0010, UNLESS OTHERWISE NOTED

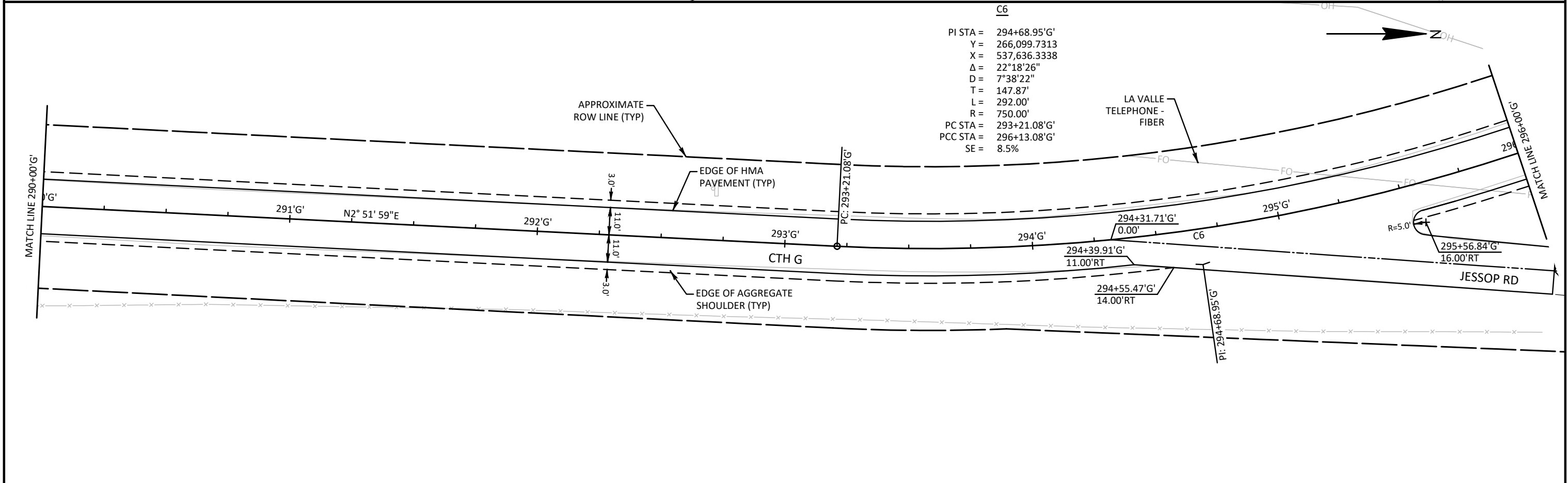
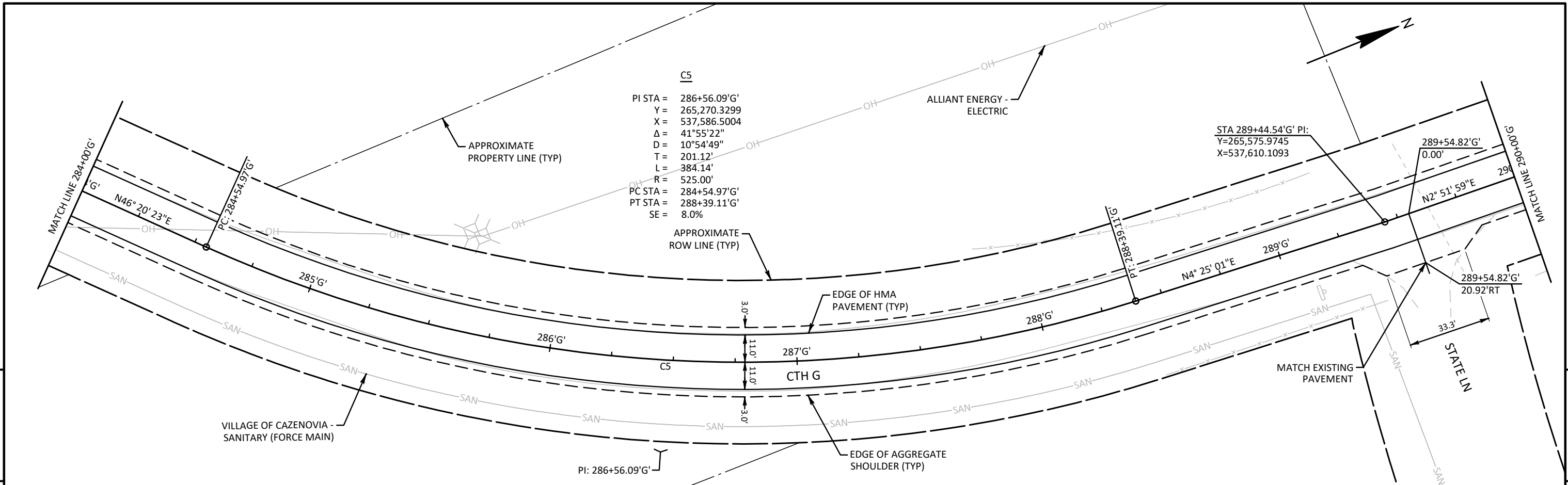


PROJECT NO: 5908-00-74	HWY: CTH G	COUNTY: SAUK	PLAN: CTH G	SHEET	E
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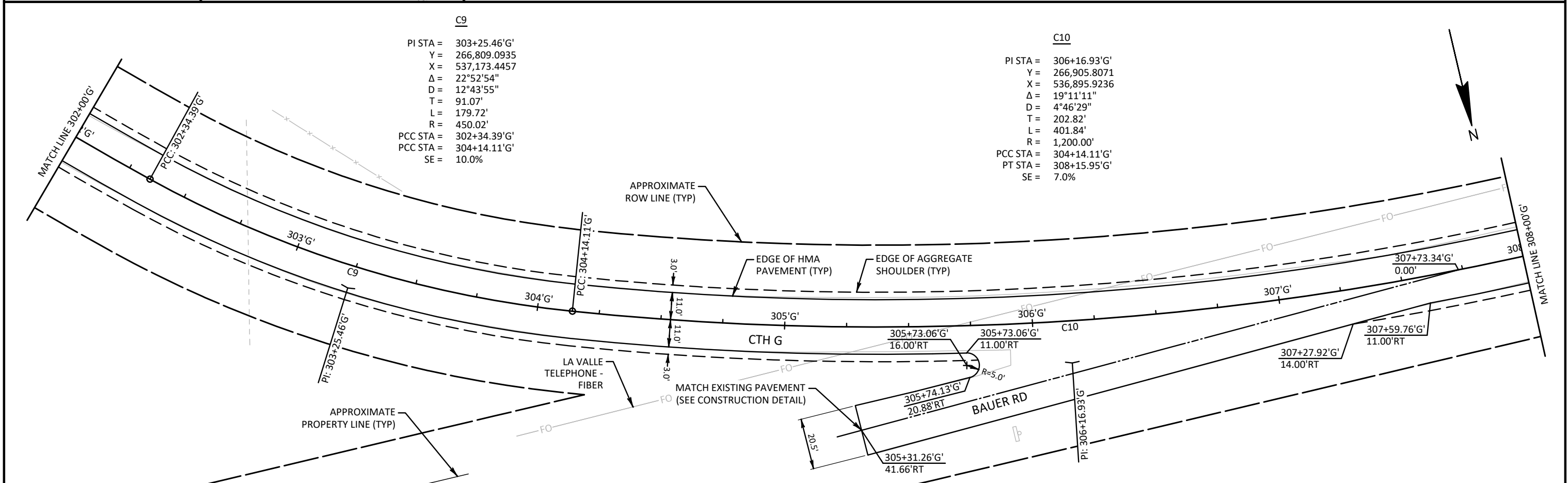
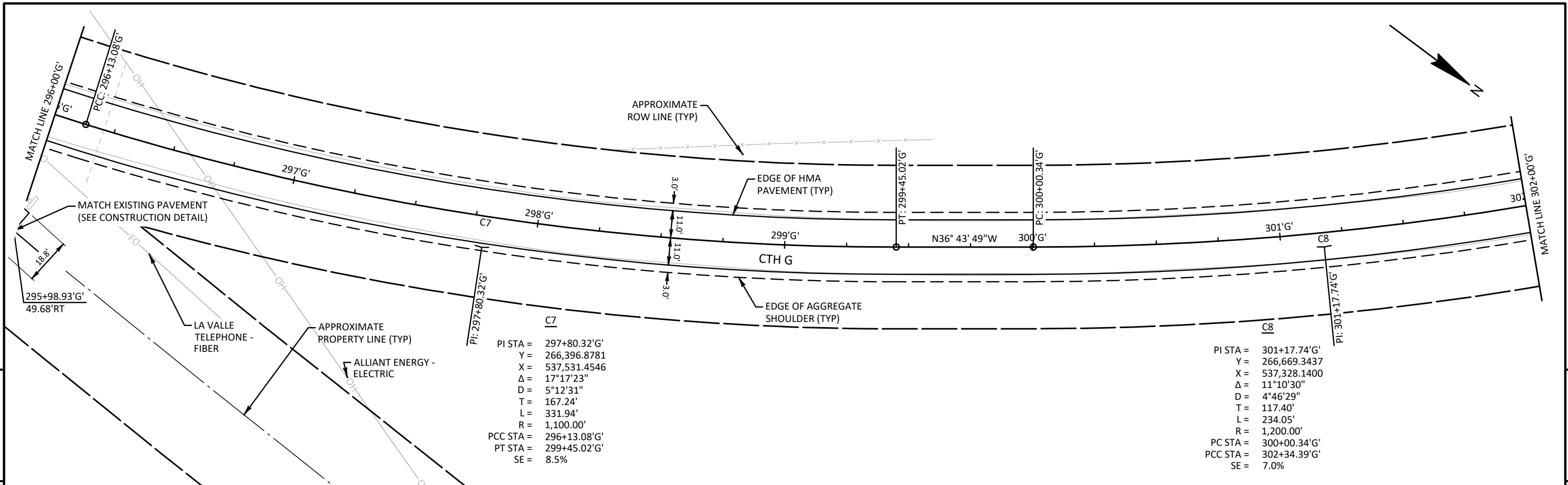


PROJECT NO: 5908-00-74	HWY: CTH G	COUNTY: SAUK	PLAN: CTH G	SHEET	E
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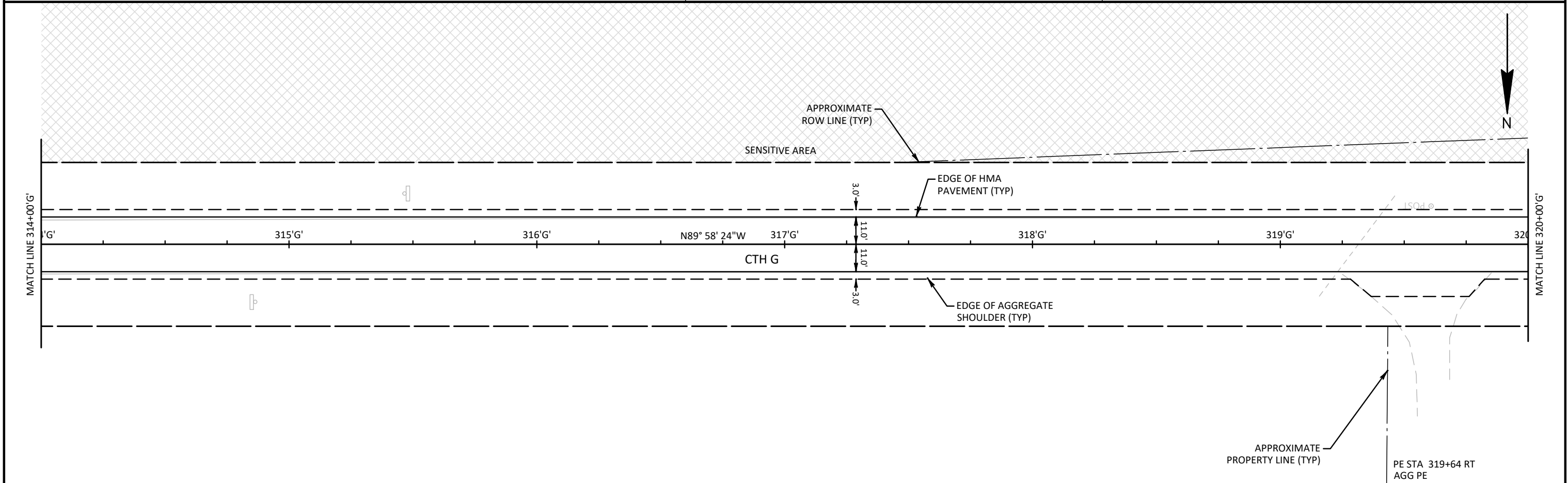
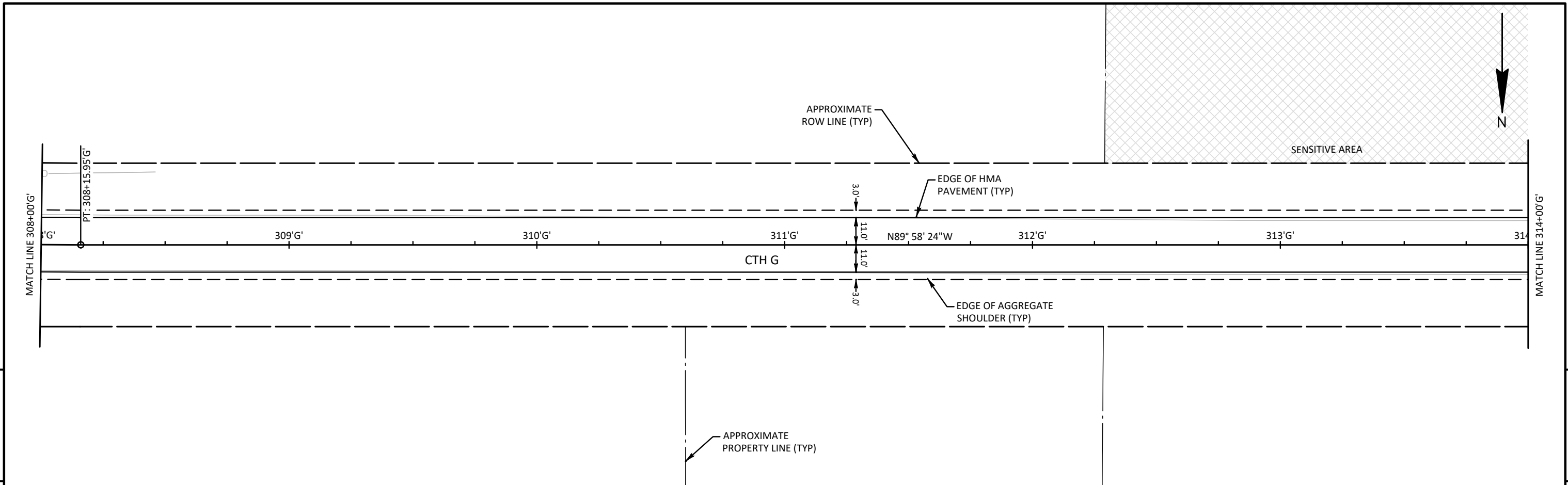


PROJECT NO: 5908-00-74	HWY: CTH G	COUNTY: SAUK	PLAN: CTH G	SHEET	E
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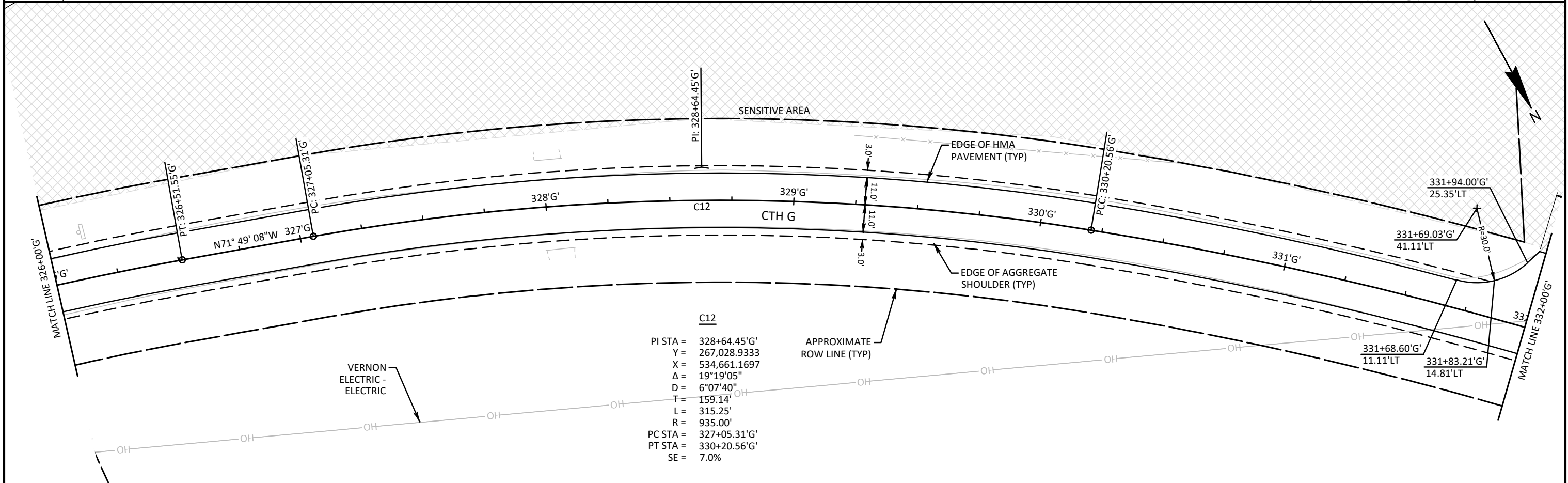
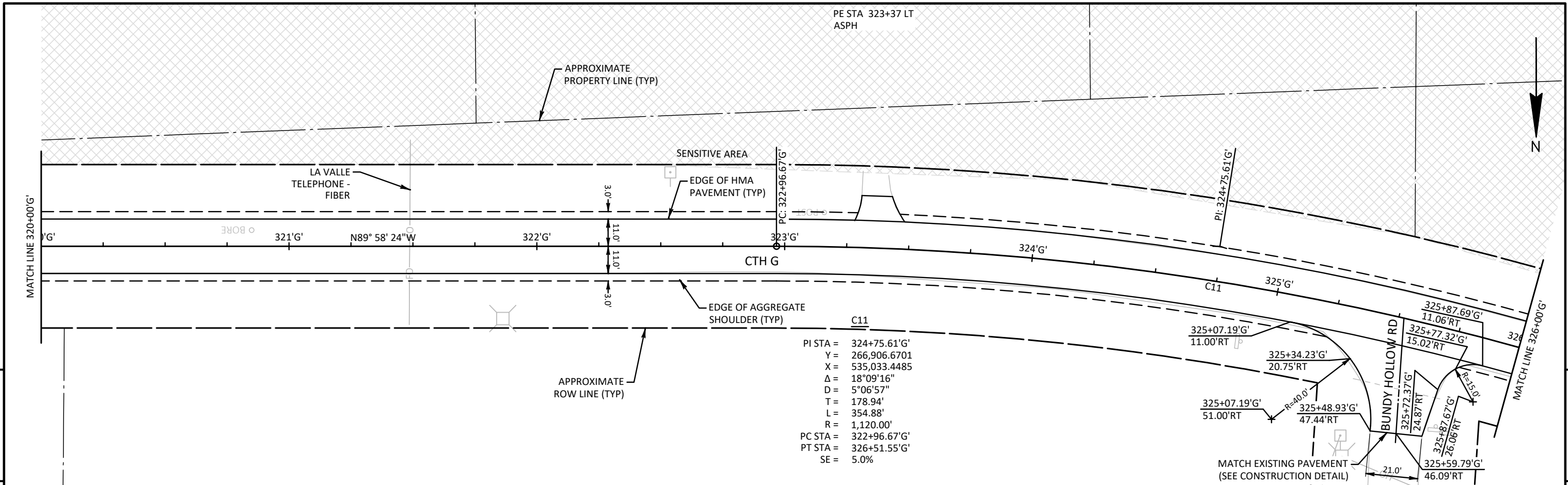




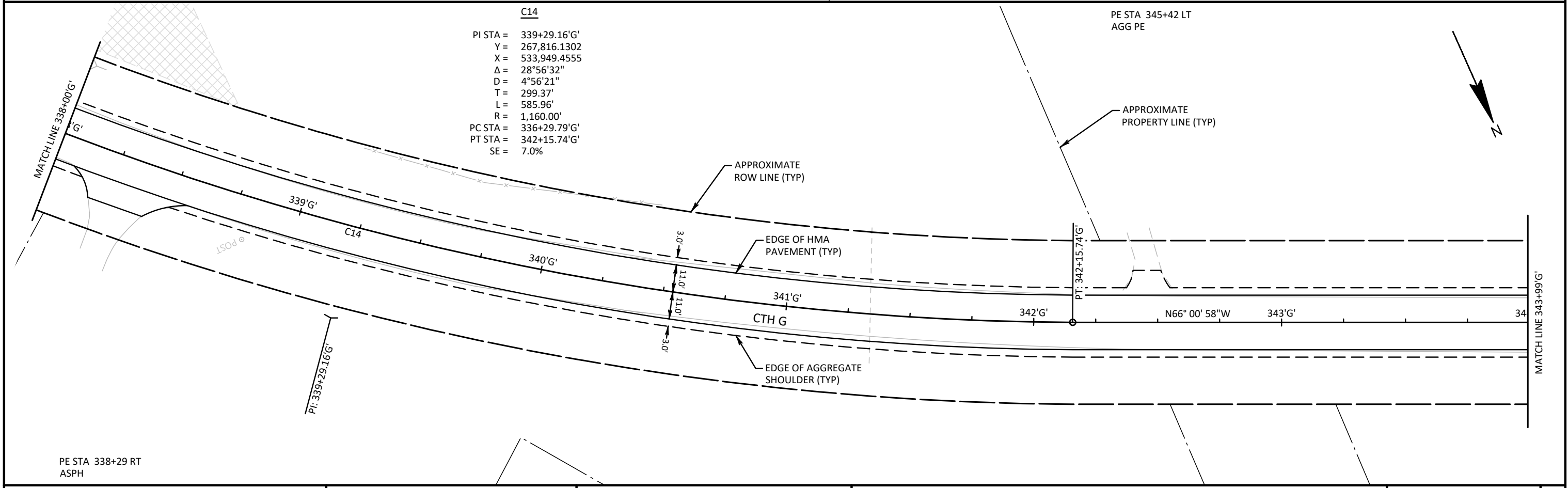
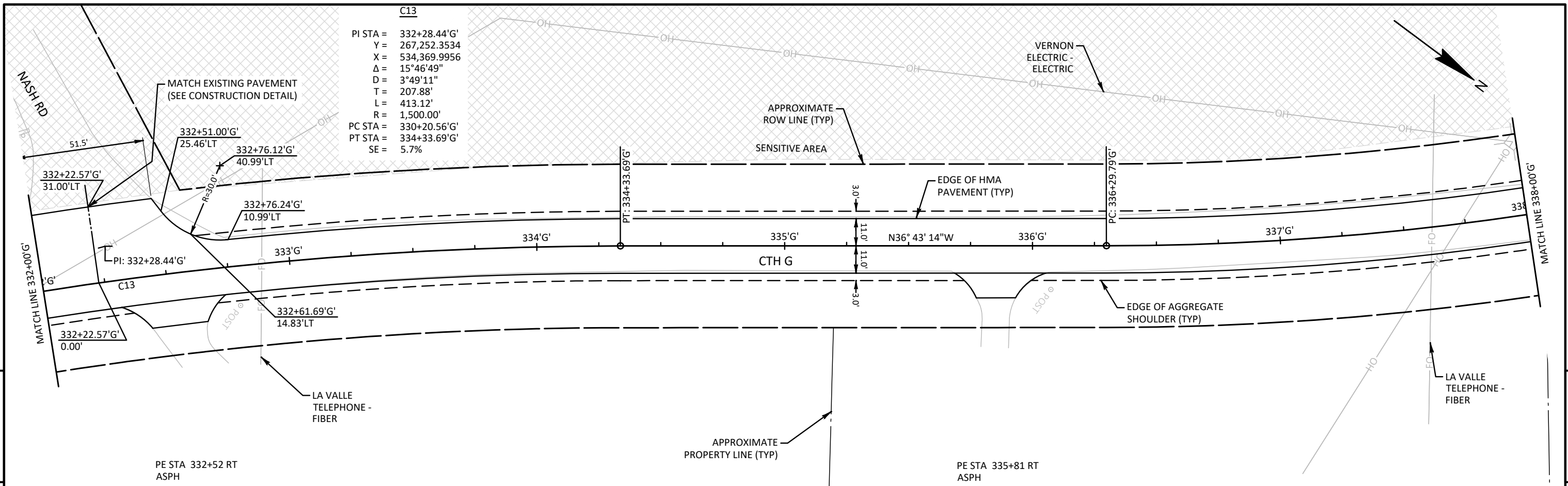
PROJECT NO: 5908-00-74	HWY: CTH G	COUNTY: SAUK	PLAN: CTH G	SHEET	E
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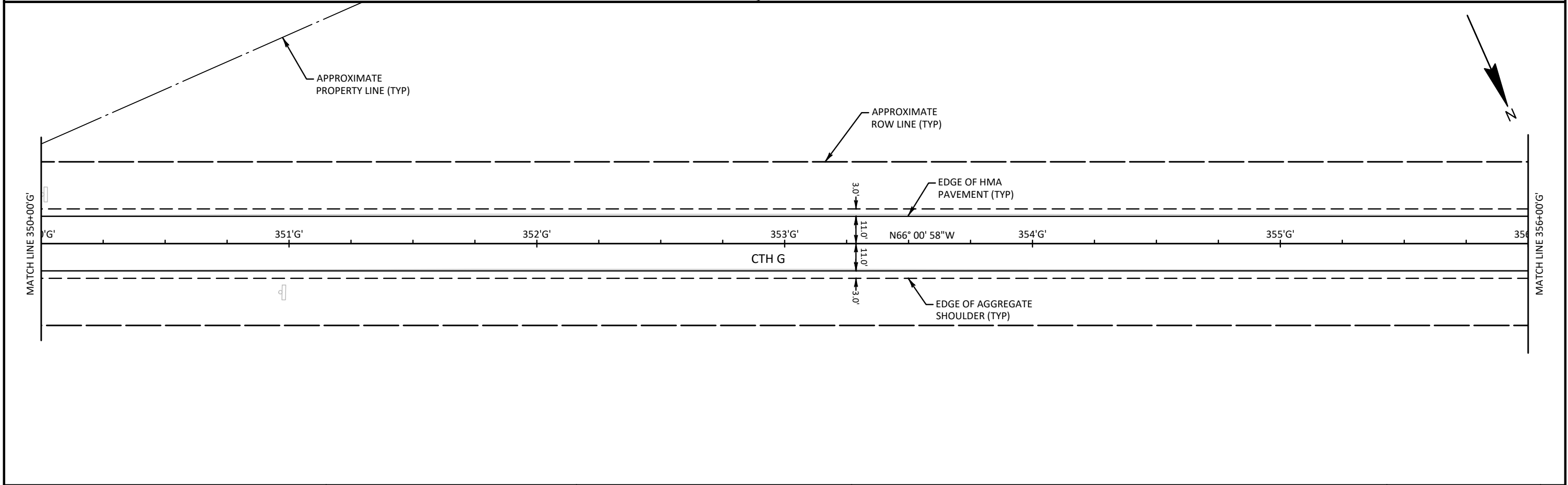
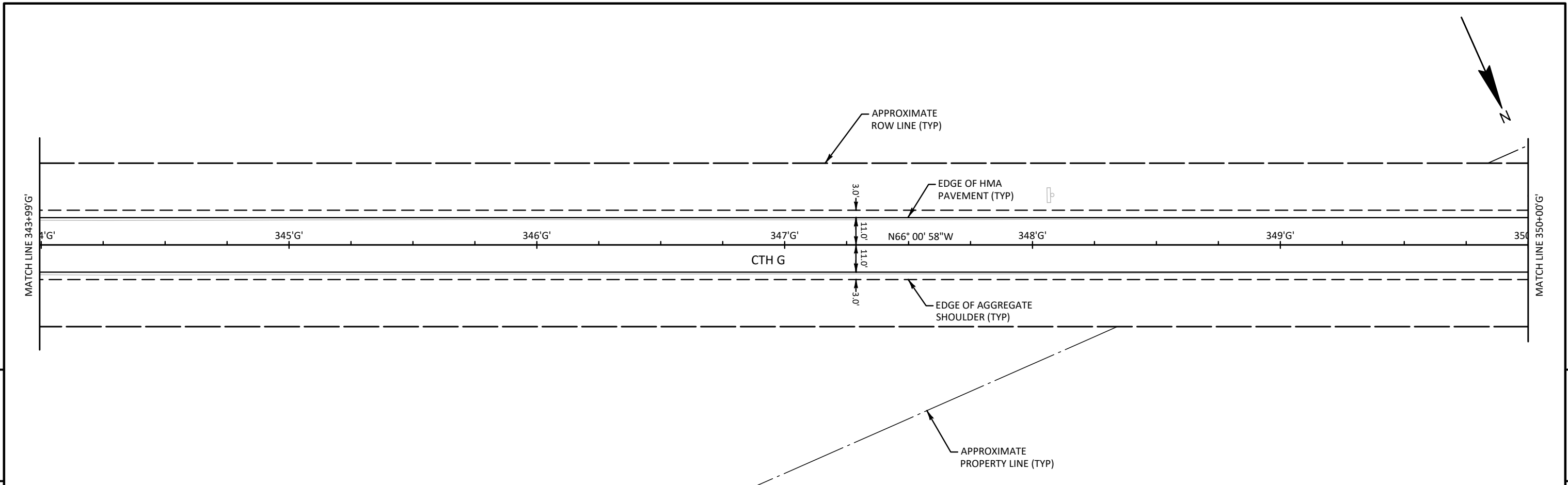
PROJECT NO: 5908-00-74	HWY: CTH G	COUNTY: SAUK	PLAN: CTH G	SHEET	E
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PROJECT NO: 5908-00-74	HWY: CTH G	COUNTY: SAUK	PLAN: CTH G	SHEET E
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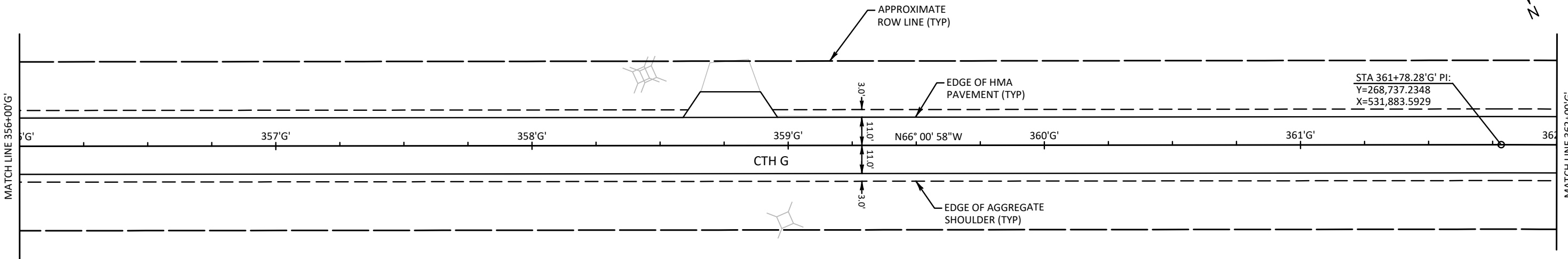
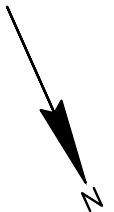


PROJECT NO: 5908-00-74	HWY: CTH G	COUNTY: SAUK	PLAN: CTH G	SHEET	E
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PROJECT NO: 5908-00-74	HWY: CTH G	COUNTY: SAUK	PLAN: CTH G	SHEET	E
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PE STA 358+79 LT  
ASPH

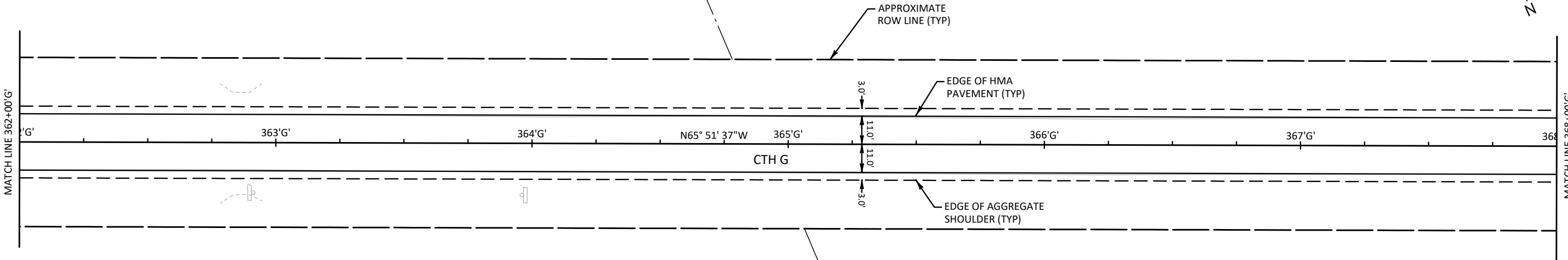
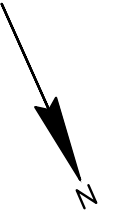


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STA 361+78.28' PI:  
Y=268,737.2348  
X=531,883.5929

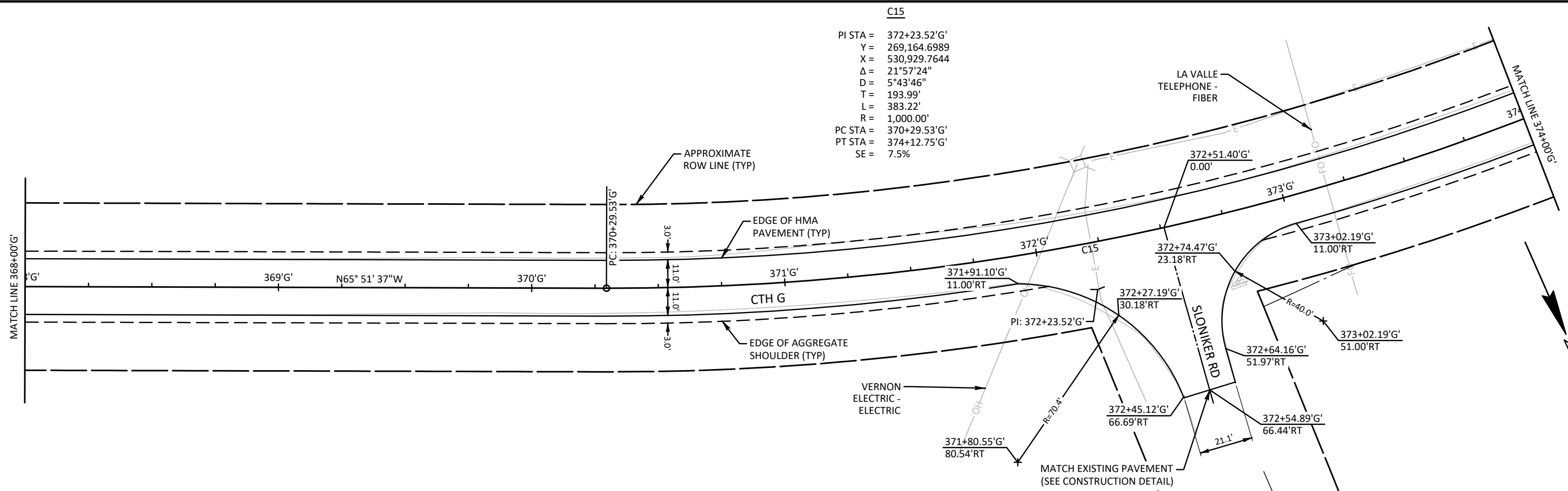
PE STA 358+79 LT  
ASPH



APPROXIMATE  
PROPERTY LINE (TYP)

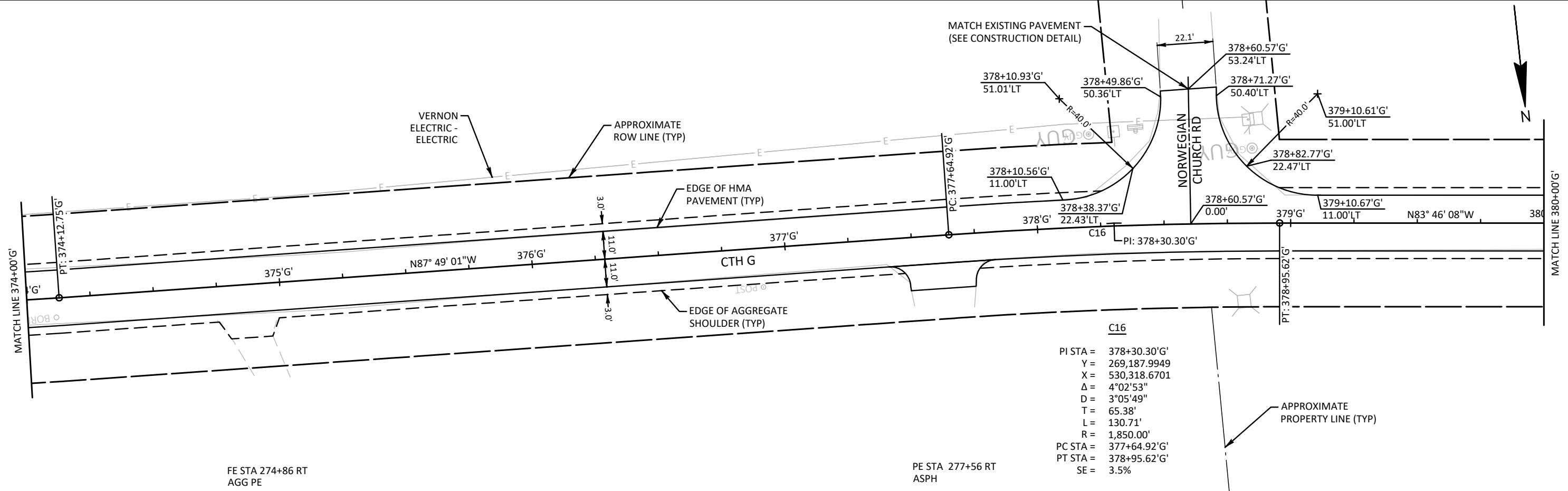
**C15**

PI STA = 372+23.52'G'  
 Y = 269,164.6989  
 X = 530,929.7644  
 Δ = 21°57'24"  
 D = 5°43'46"  
 T = 193.99'  
 L = 383.22'  
 R = 1,000.00'  
 PC STA = 370+29.53'G'  
 PT STA = 374+12.75'G'  
 SE = 7.5%



**C16**

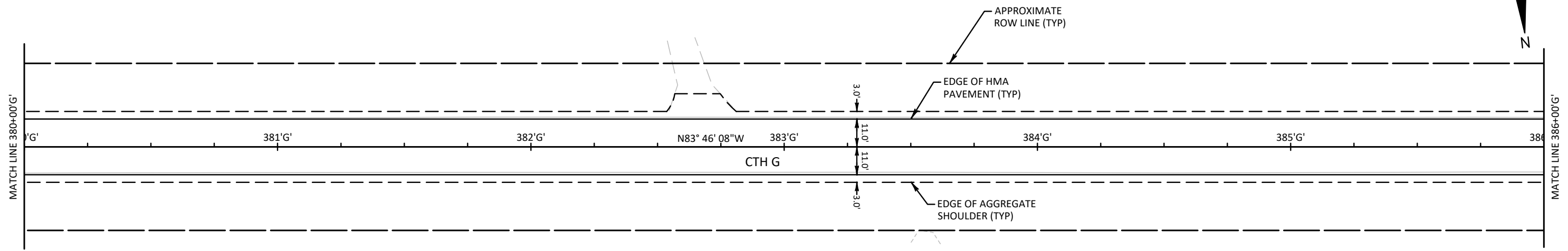
PI STA = 378+30.30'G'  
 Y = 269,187.9949  
 X = 530,318.6701  
 Δ = 4°02'53"  
 D = 3°05'49"  
 T = 65.38'  
 L = 130.71'  
 R = 1,850.00'  
 PC STA = 377+64.92'G'  
 PT STA = 378+95.62'G'  
 SE = 3.5%



FE STA 274+86 RT  
 AGG PE

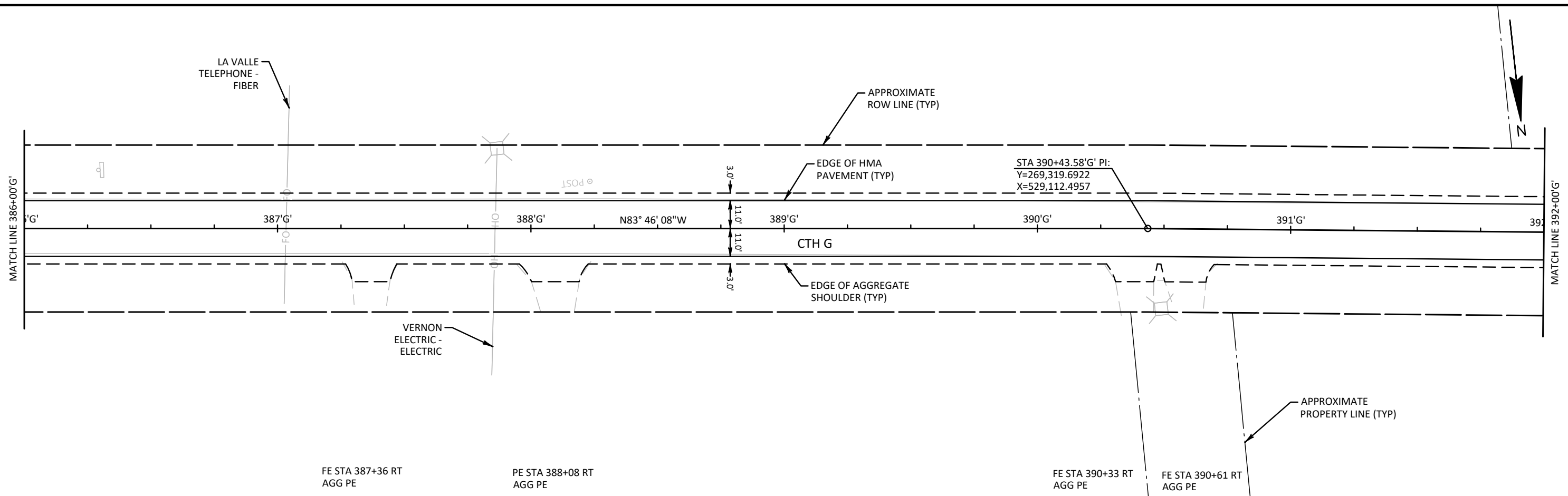
PE STA 277+56 RT  
 ASPH

PE STA 382+69 LT  
AGG PE



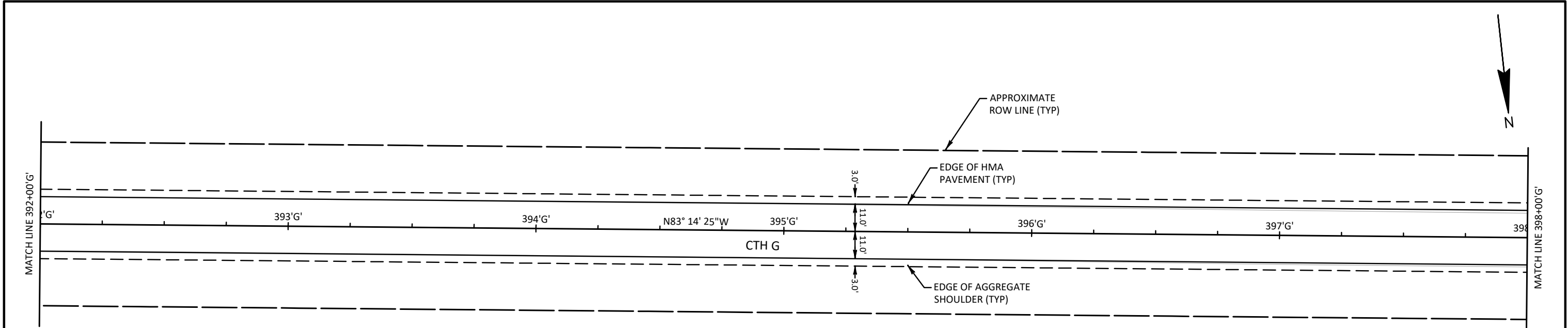
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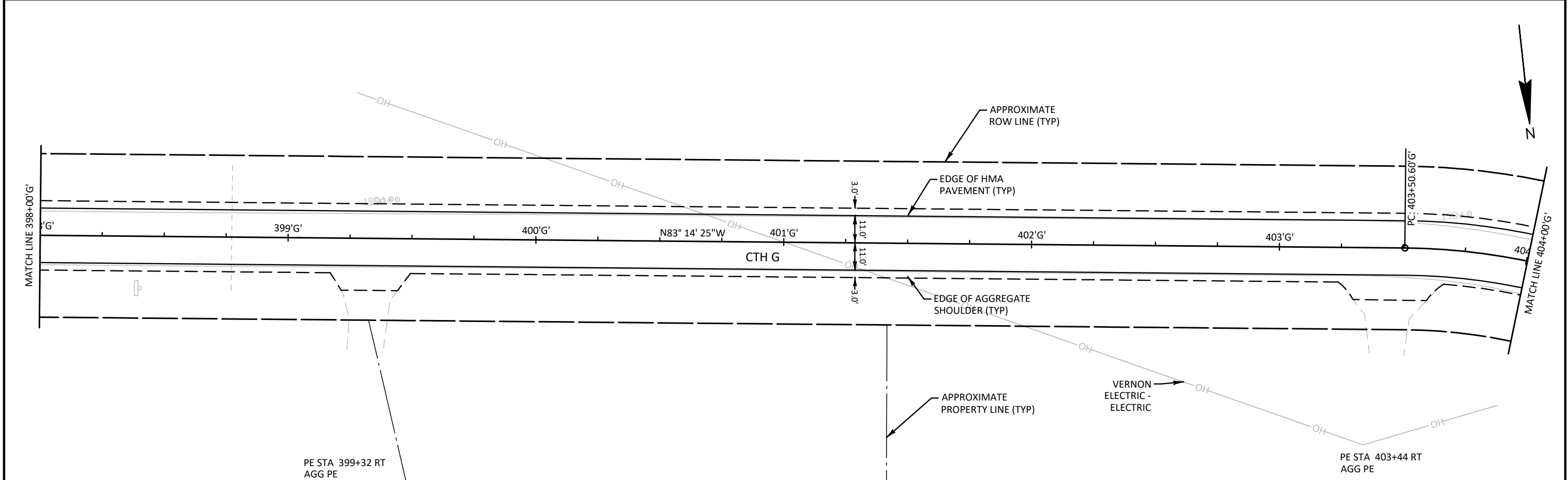
PROJECT NO: 5908-00-74	HWY: CTH G	COUNTY: SAUK	PLAN: CTH G	SHEET	E
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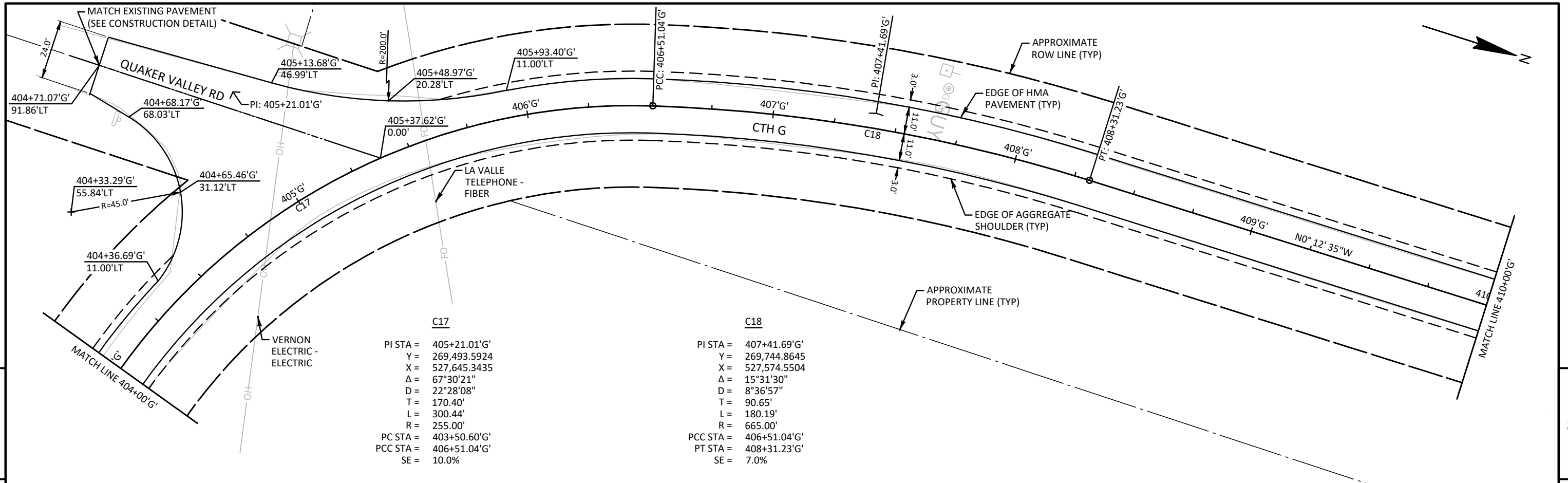


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5



PROJECT NO: 5908-00-74	HWY: CTH G	COUNTY: SAUK	PLAN: CTH G	SHEET	E
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5

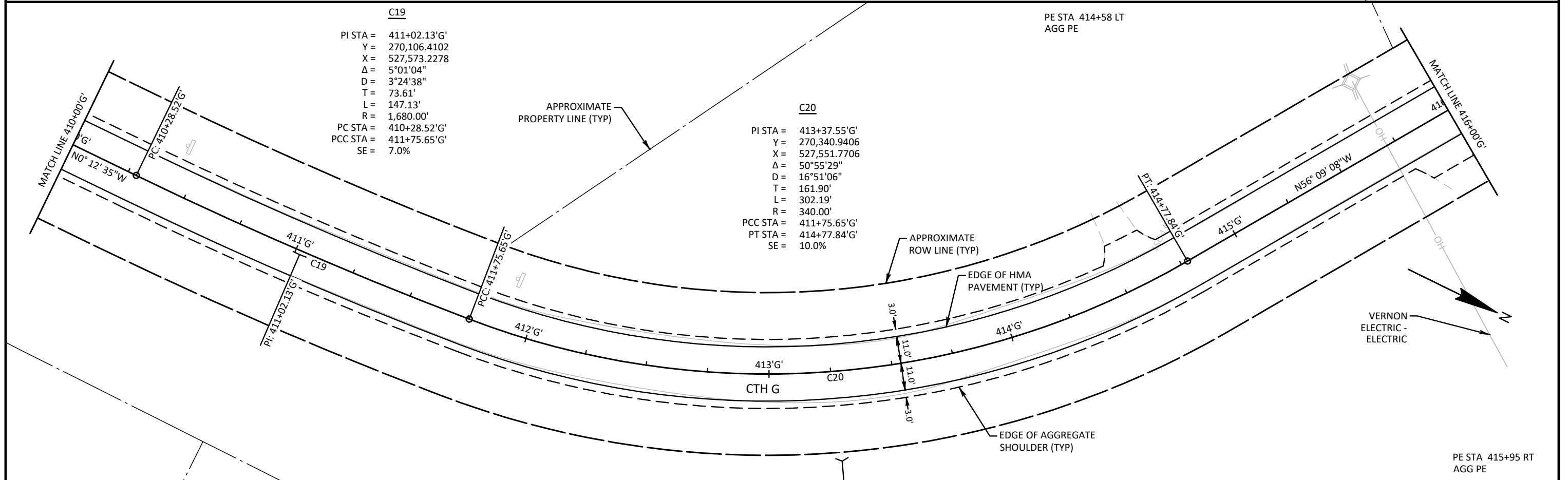
5

**C17**  
 PI STA = 405+21.01'G'  
 Y = 269,493.5924  
 X = 527,645.3435  
 Δ = 67°30'21"  
 D = 22°28'08"  
 T = 170.40'  
 L = 300.44'  
 R = 255.00'  
 PC STA = 403+50.60'G'  
 PCC STA = 406+51.04'G'  
 SE = 10.0%

**C18**  
 PI STA = 407+41.69'G'  
 Y = 269,744.8645  
 X = 527,574.5504  
 Δ = 15°31'30"  
 D = 8°36'57"  
 T = 90.65'  
 L = 180.19'  
 R = 665.00'  
 PCC STA = 406+51.04'G'  
 PT STA = 408+31.23'G'  
 SE = 7.0%

**C19**  
 PI STA = 411+02.13'G'  
 Y = 270,106.4102  
 X = 527,573.2278  
 Δ = 5°01'04"  
 D = 3°24'38"  
 T = 73.61'  
 L = 147.13'  
 R = 1,680.00'  
 PC STA = 410+28.52'G'  
 PCC STA = 411+75.65'G'  
 SE = 7.0%

**C20**  
 PI STA = 413+37.55'G'  
 Y = 270,340.9406  
 X = 527,551.7706  
 Δ = 50°55'29"  
 D = 16°51'06"  
 T = 161.90'  
 L = 302.19'  
 R = 340.00'  
 PCC STA = 411+75.65'G'  
 PT STA = 414+77.84'G'  
 SE = 10.0%



PROJECT NO: 5908-00-74

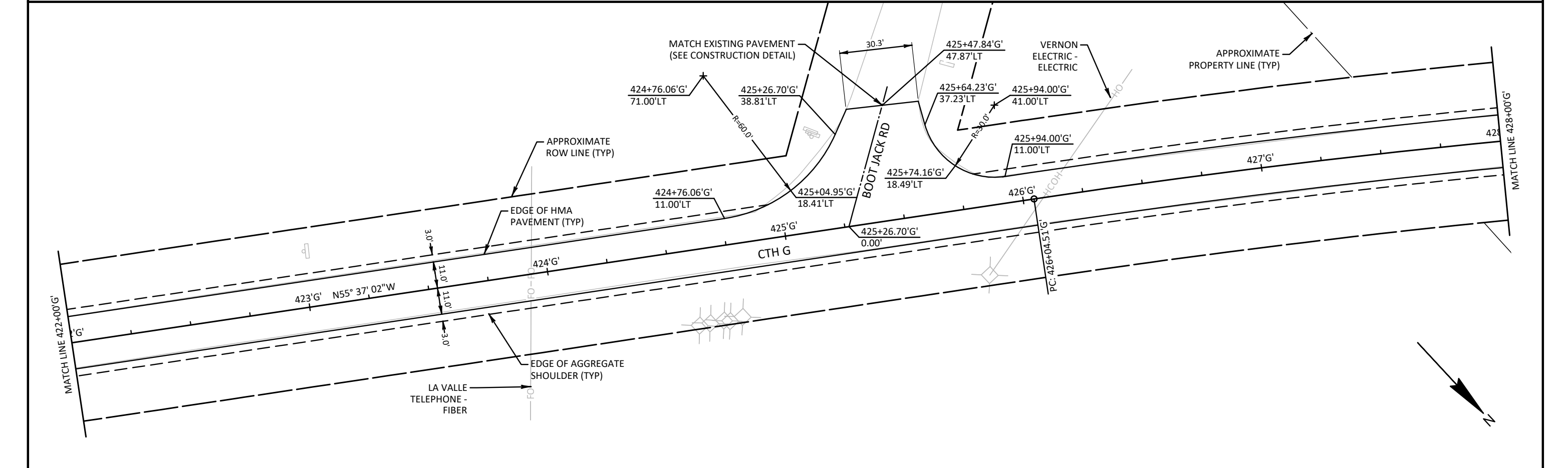
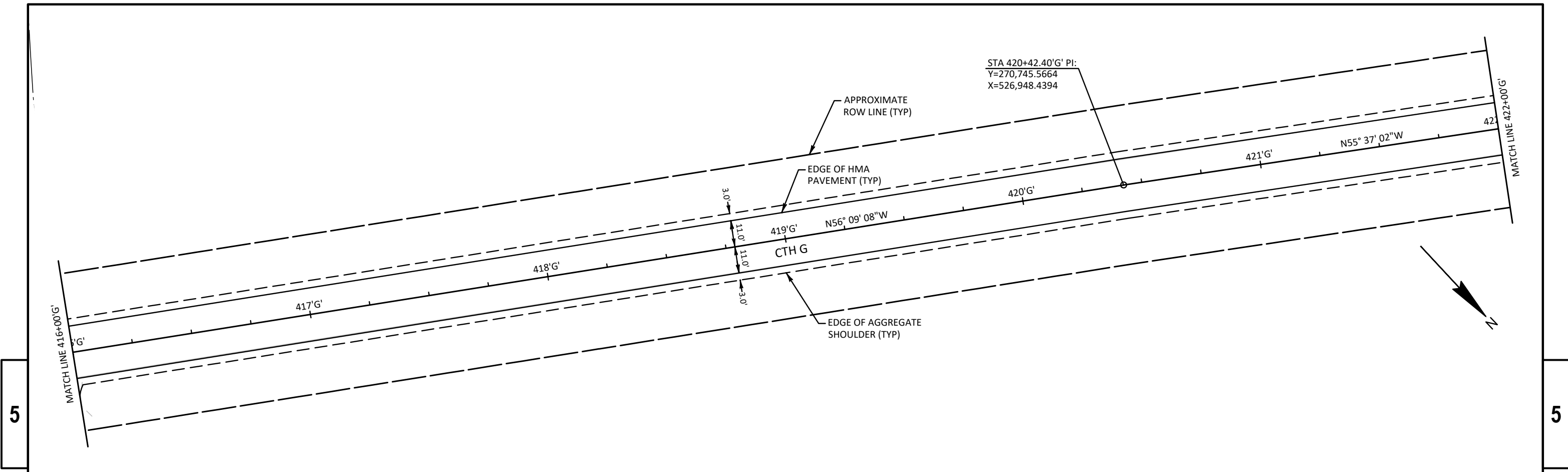
HWY: CTH G

COUNTY: SAUK

PLAN: CTH G

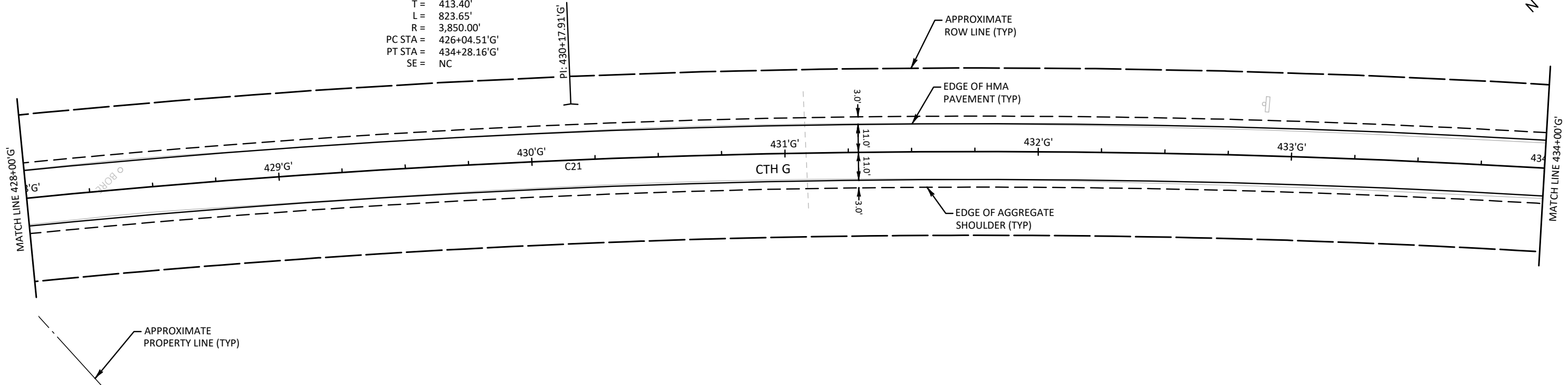
SHEET

E



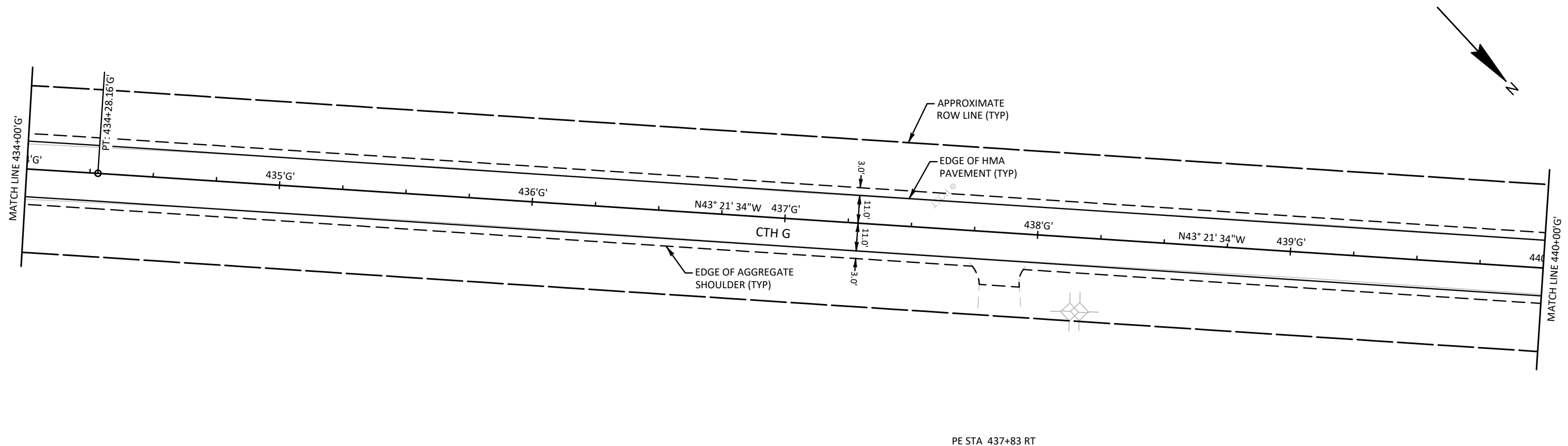
PROJECT NO: 5908-00-74	HWY: CTH G	COUNTY: SAUK	PLAN: CTH G	SHEET	<b>E</b>
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C21  
 PI STA = 430+17.91'G'  
 Y = 271,296.4597  
 X = 526,143.3620  
 Δ = 12°15'27"  
 D = 1°29'18"  
 T = 413.40'  
 L = 823.65'  
 R = 3,850.00'  
 PC STA = 426+04.51'G'  
 PT STA = 434+28.16'G'  
 SE = NC



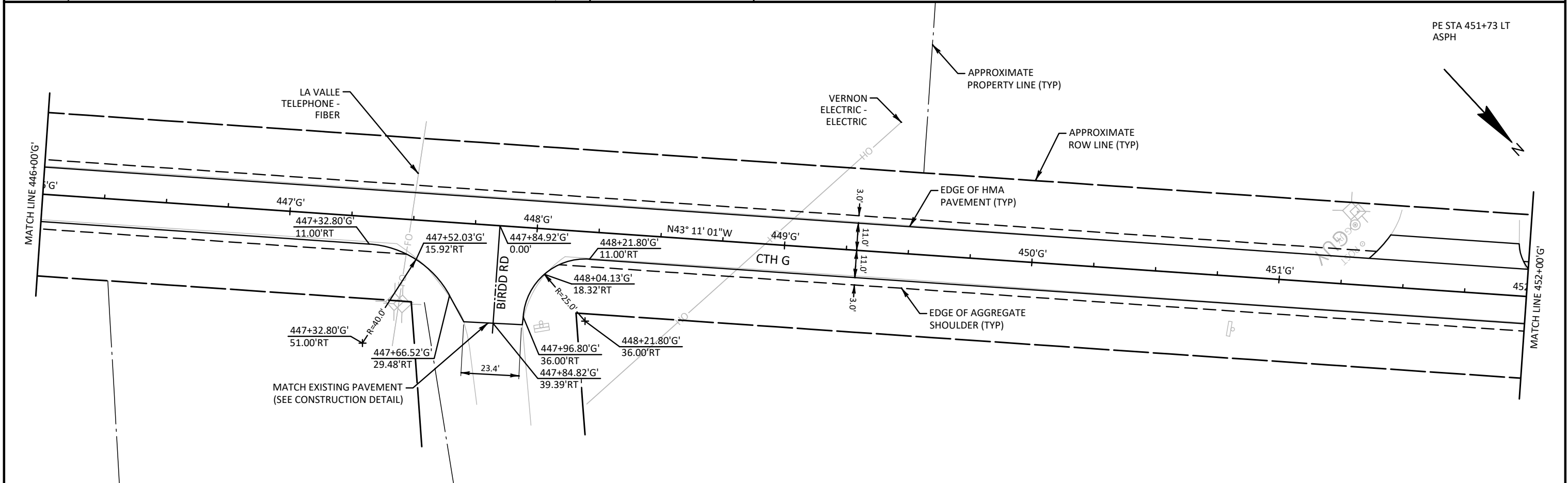
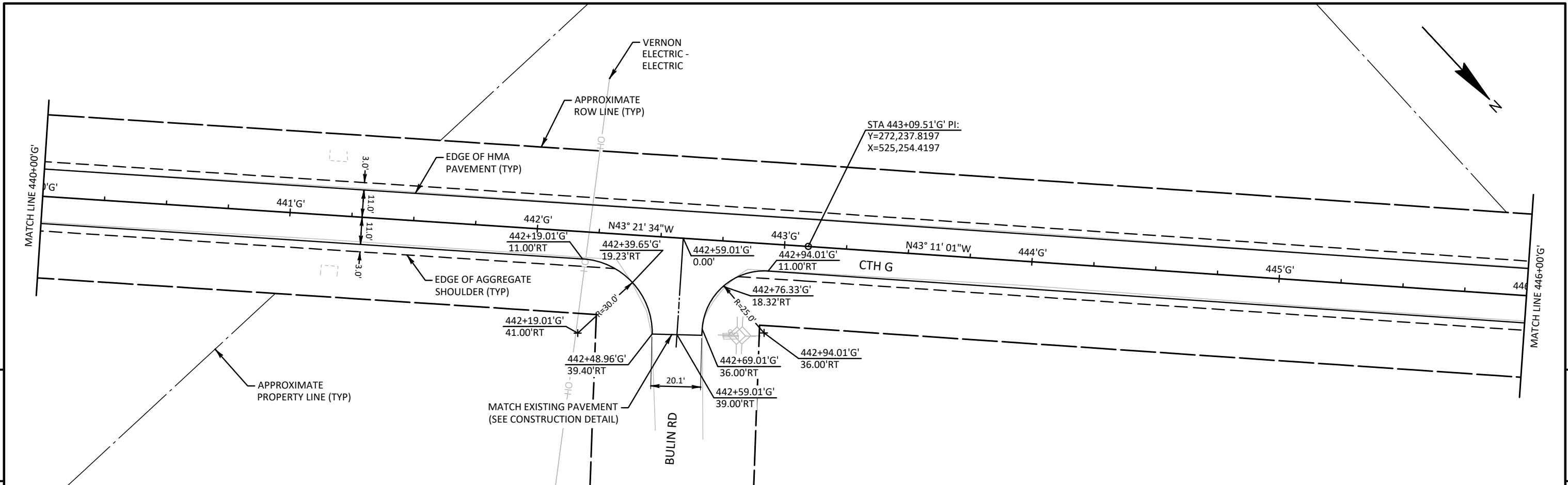
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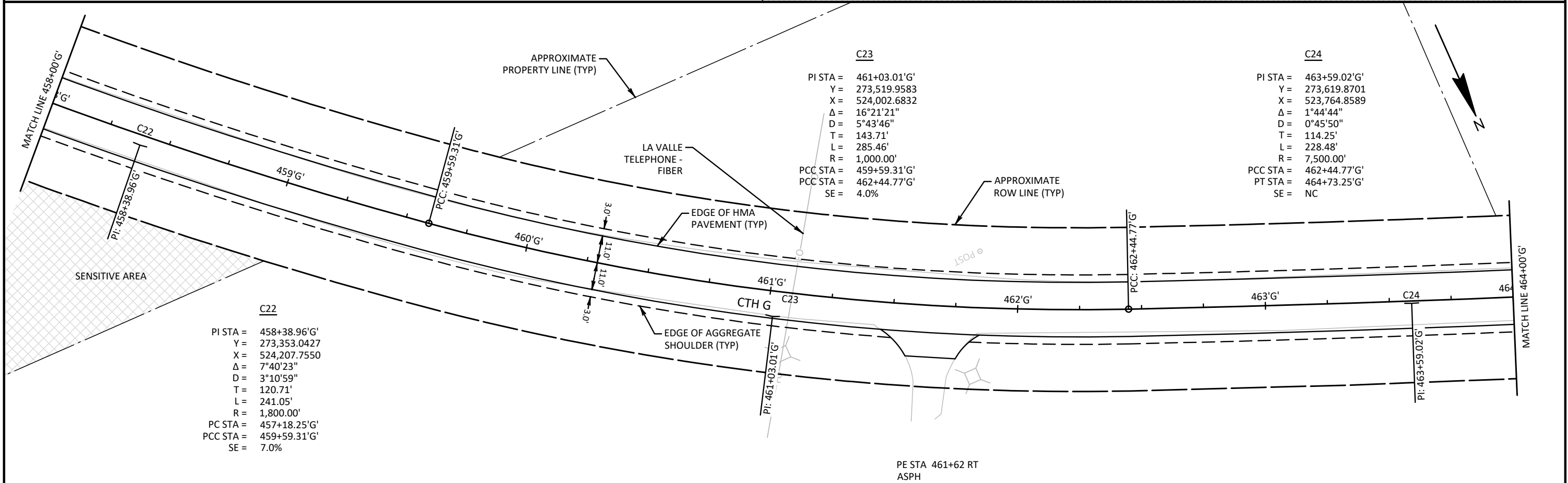
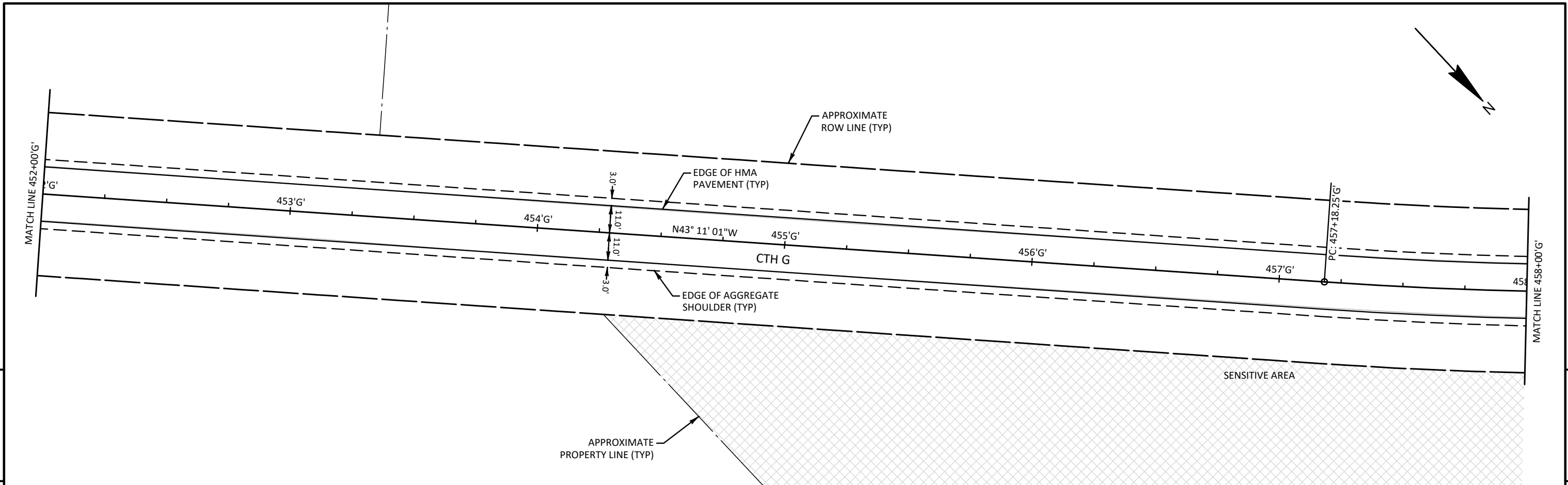


PE STA 437+83 RT  
 AGG PE

PROJECT NO: 5908-00-74	HWY: CTH G	COUNTY: SAUK	PLAN: CTH G	SHEET	E
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PROJECT NO: 5908-00-74	HWY: CTH G	COUNTY: SAUK	PLAN: CTH G	SHEET	E
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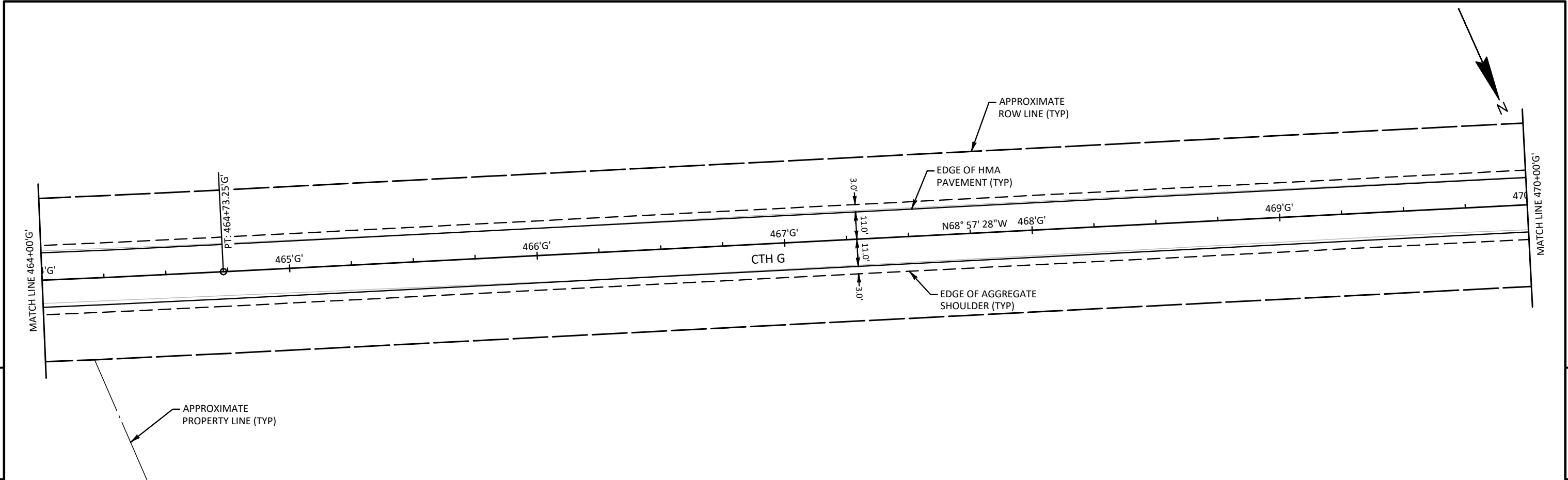


**C22**  
 PI STA = 458+38.96'G'  
 Y = 273,353.0427  
 X = 524,207.7550  
 Δ = 7°40'23"  
 D = 3°10'59"  
 T = 120.71'  
 L = 241.05'  
 R = 1,800.00'  
 PC STA = 457+18.25'G'  
 PCC STA = 459+59.31'G'  
 SE = 7.0%

**C23**  
 PI STA = 461+03.01'G'  
 Y = 273,519.9583  
 X = 524,002.6832  
 Δ = 16°21'21"  
 D = 5°43'46"  
 T = 143.71'  
 L = 285.46'  
 R = 1,000.00'  
 PCC STA = 459+59.31'G'  
 PCC STA = 462+44.77'G'  
 SE = 4.0%

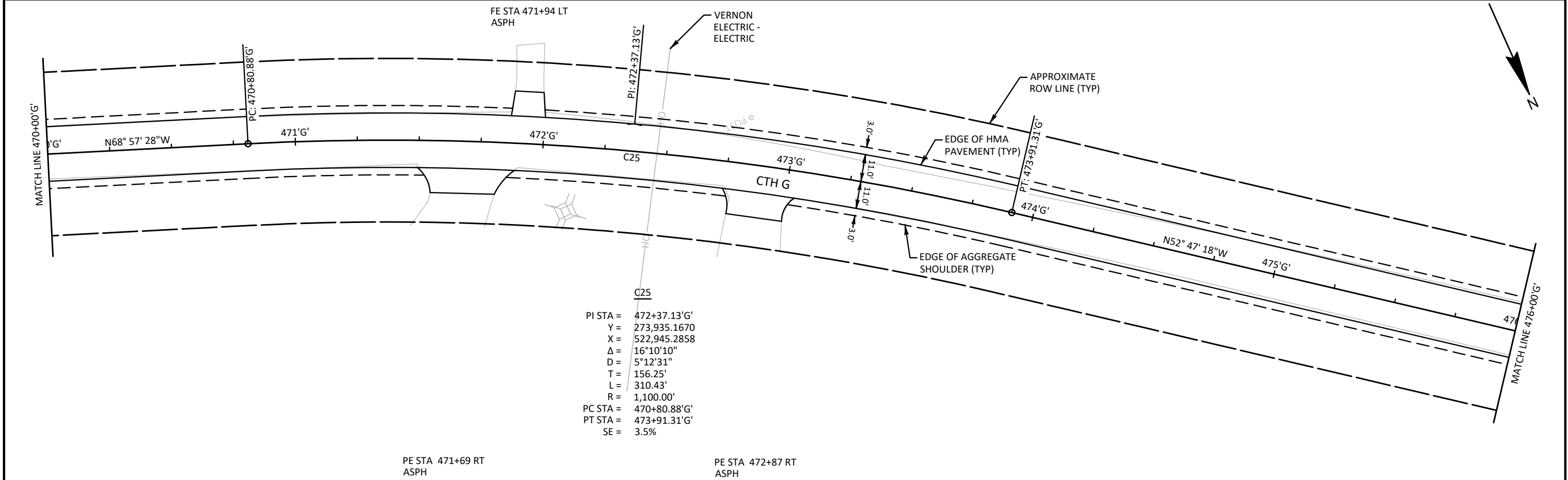
**C24**  
 PI STA = 463+59.02'G'  
 Y = 273,619.8701  
 X = 523,764.8589  
 Δ = 1°44'44"  
 D = 0°45'50"  
 T = 114.25'  
 L = 228.48'  
 R = 7,500.00'  
 PCC STA = 462+44.77'G'  
 PT STA = 464+73.25'G'  
 SE = NC

PE STA 461+62 RT  
 ASPH



5

5



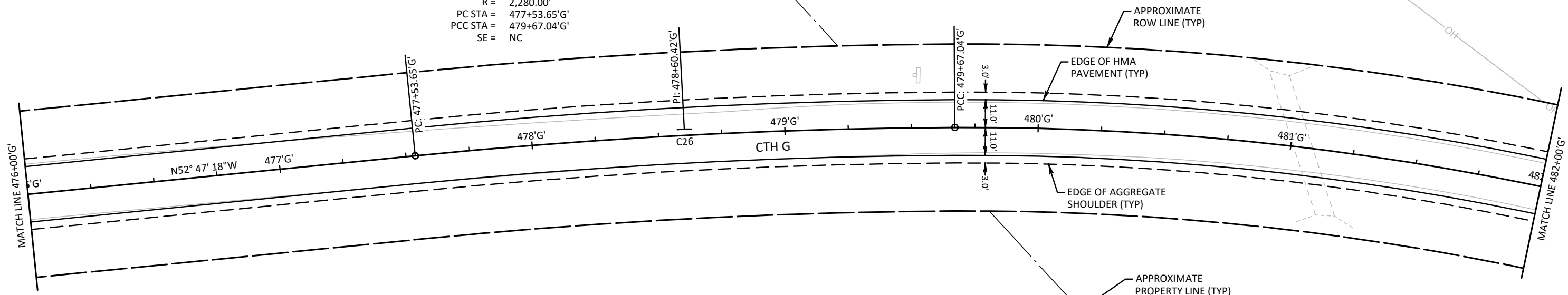
5

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PROJECT NO: 5908-00-74      HWY: CTH G      COUNTY: SAUK      PLAN: CTH G      SHEET      E

**C26**  
 PI STA = 478+60.42'G'  
 Y = 274,313.3654  
 X = 522,447.2378  
 Δ = 5°21'45"  
 D = 2°30'47"  
 T = 106.77'  
 L = 213.39'  
 R = 2,280.00'  
 PC STA = 477+53.65'G'  
 PCC STA = 479+67.04'G'  
 SE = NC

VERNON  
ELECTRIC -  
ELECTRIC

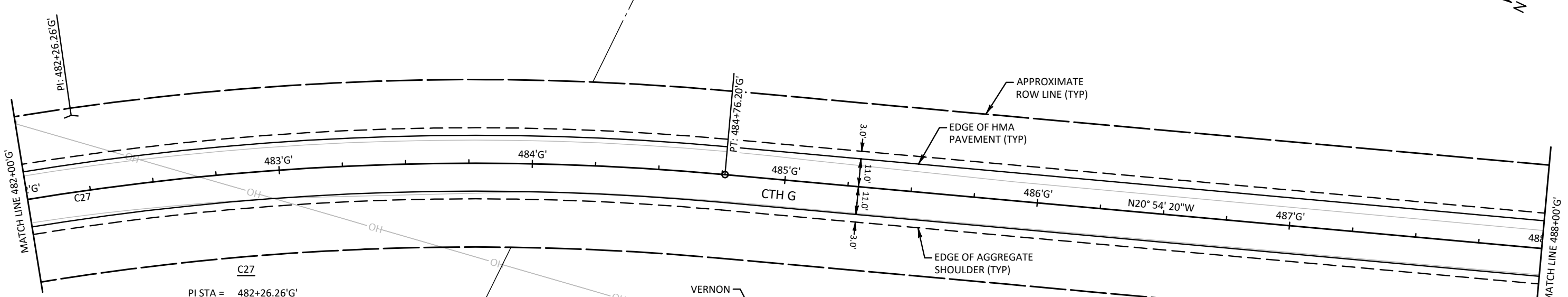


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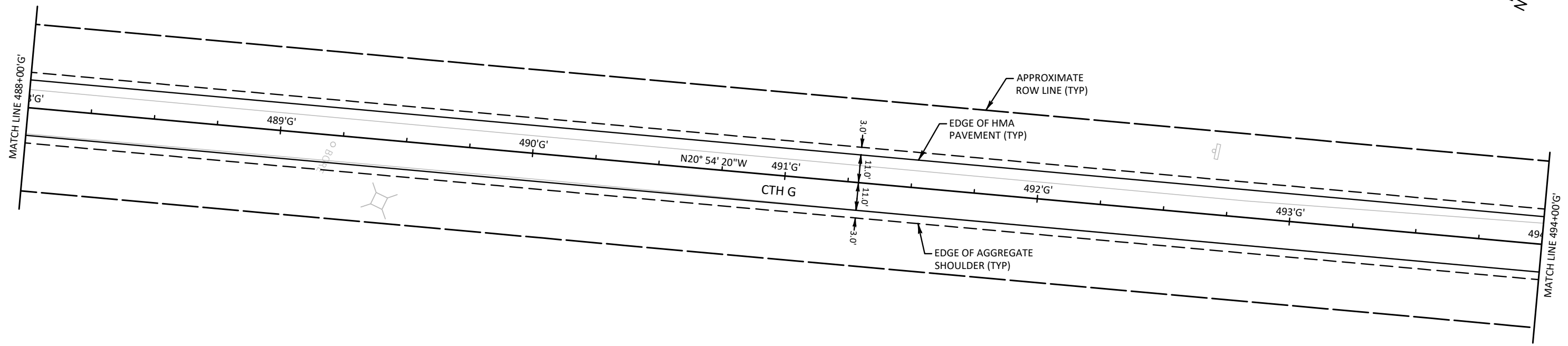
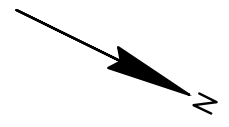
5

**C27**  
 PI STA = 482+26.26'G'  
 Y = 274,560.9779  
 X = 522,177.7171  
 Δ = 26°31'13"  
 D = 5°12'31"  
 T = 259.22'  
 L = 509.15'  
 R = 1,100.00'  
 PCC STA = 479+67.04'G'  
 PT STA = 484+76.20'G'  
 SE = 7.4%

VERNON  
ELECTRIC -  
ELECTRIC

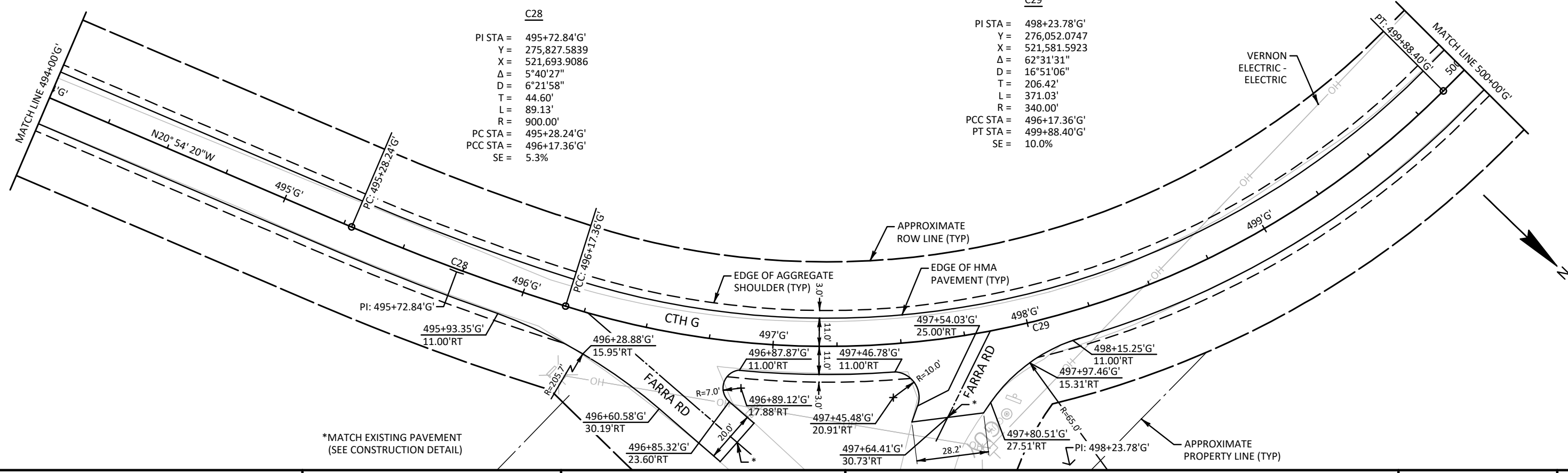






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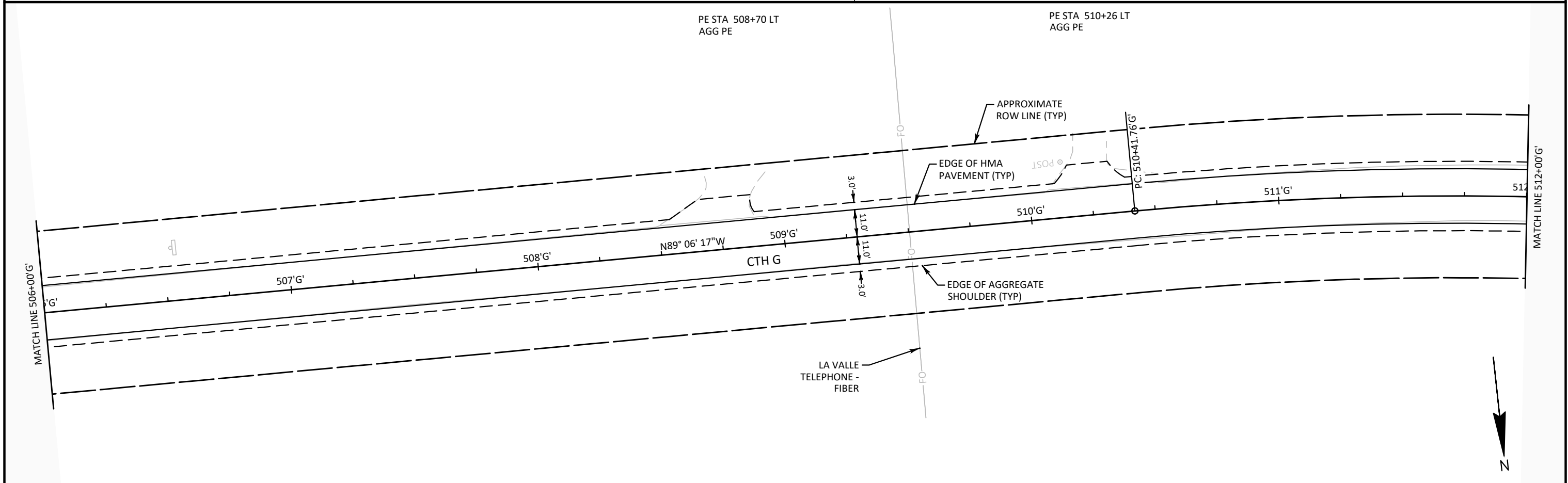
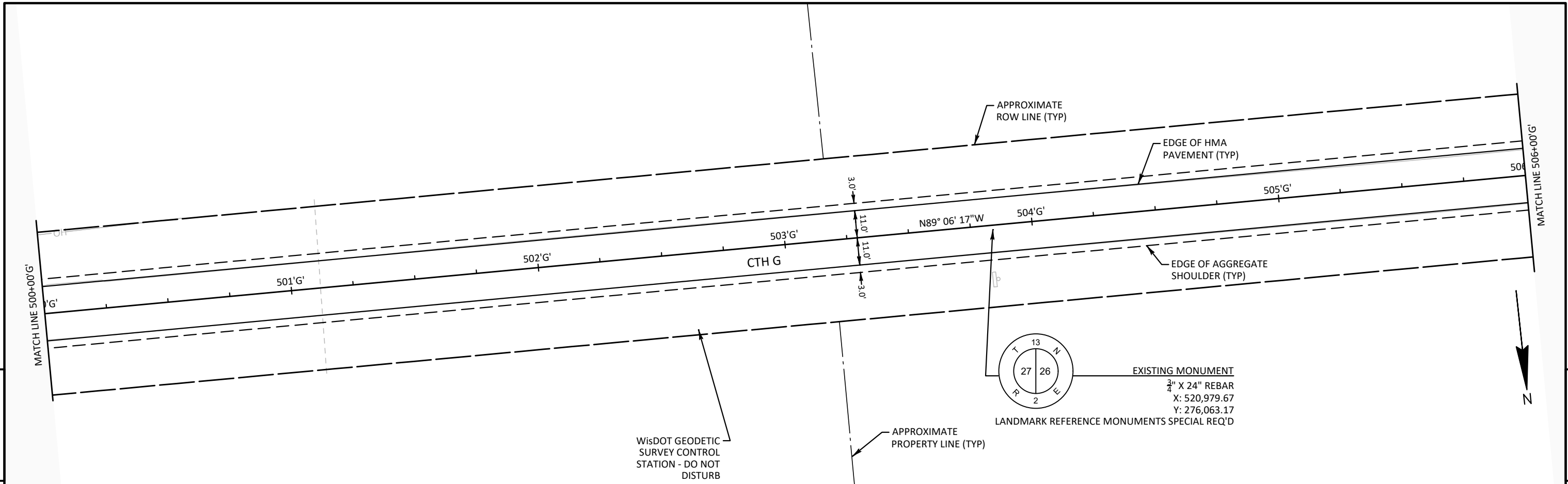
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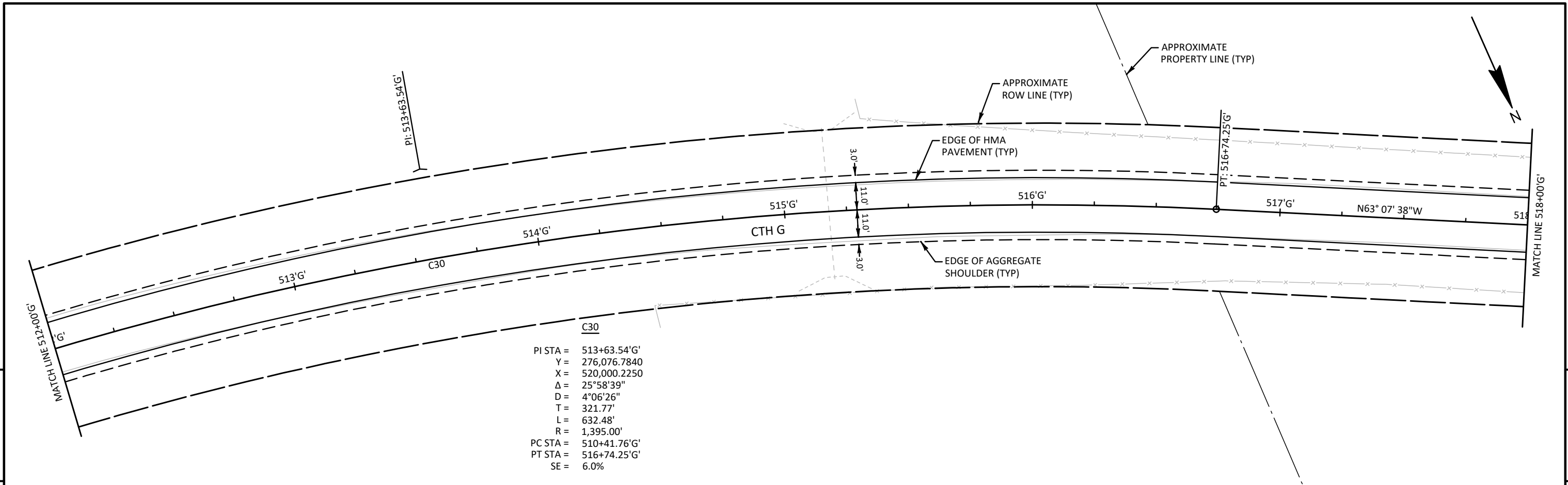
**C28**  
 PI STA = 495+72.84'G'  
 Y = 275,827.5839  
 X = 521,693.9086  
 Δ = 5°40'27"  
 D = 6°21'58"  
 T = 44.60'  
 L = 89.13'  
 R = 900.00'  
 PC STA = 495+28.24'G'  
 PCC STA = 496+17.36'G'  
 SE = 5.3%

**C29**  
 PI STA = 498+23.78'G'  
 Y = 276,052.0747  
 X = 521,581.5923  
 Δ = 62°31'31"  
 D = 16°51'06"  
 T = 206.42'  
 L = 371.03'  
 R = 340.00'  
 PCC STA = 496+17.36'G'  
 PT STA = 499+88.40'G'  
 SE = 10.0%

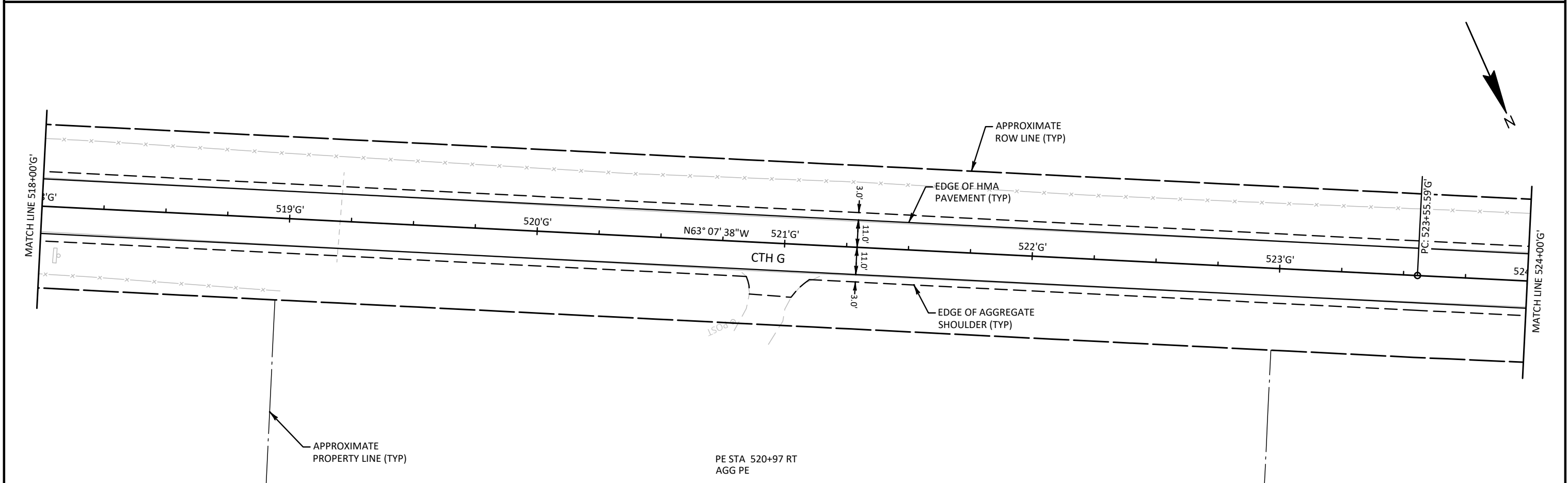
\*MATCH EXISTING PAVEMENT (SEE CONSTRUCTION DETAIL)



PROJECT NO: 5908-00-74	HWY: CTH G	COUNTY: SAUK	PLAN: CTH G	SHEET	E
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**C30**  
 PI STA = 513+63.54'G'  
 Y = 276,076.7840  
 X = 520,000.2250  
 Δ = 25°58'39"  
 D = 4°06'26"  
 T = 321.77'  
 L = 632.48'  
 R = 1,395.00'  
 PC STA = 510+41.76'G'  
 PT STA = 516+74.25'G'  
 SE = 6.0%

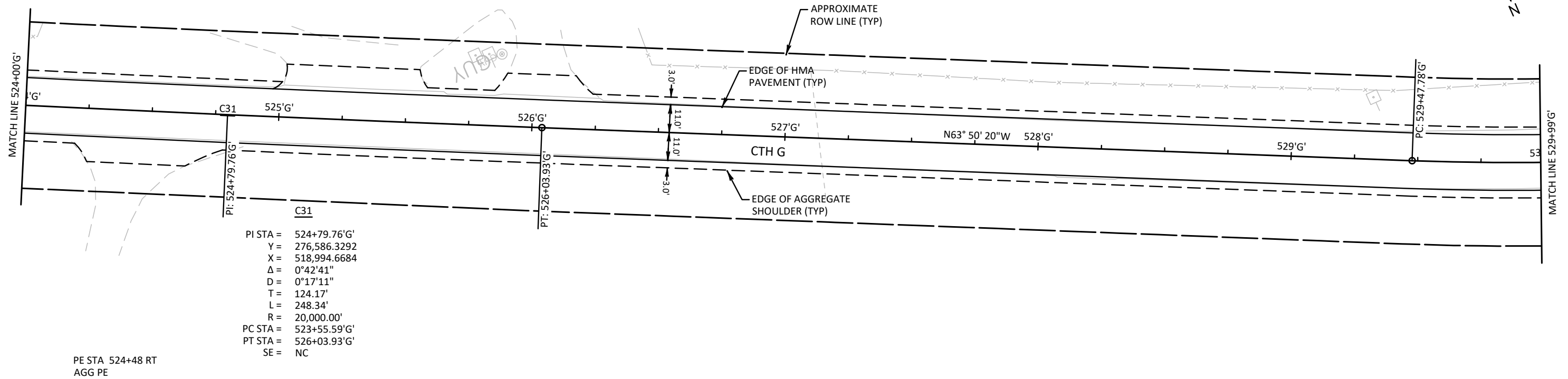


PE STA 520+97 RT  
 AGG PE

PROJECT NO: 5908-00-74	HWY: CTH G	COUNTY: SAUK	PLAN: CTH G	SHEET	<b>E</b>
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PE STA 525+27 LT  
AGG PE

PE STA 526+06 LT  
AGG PE

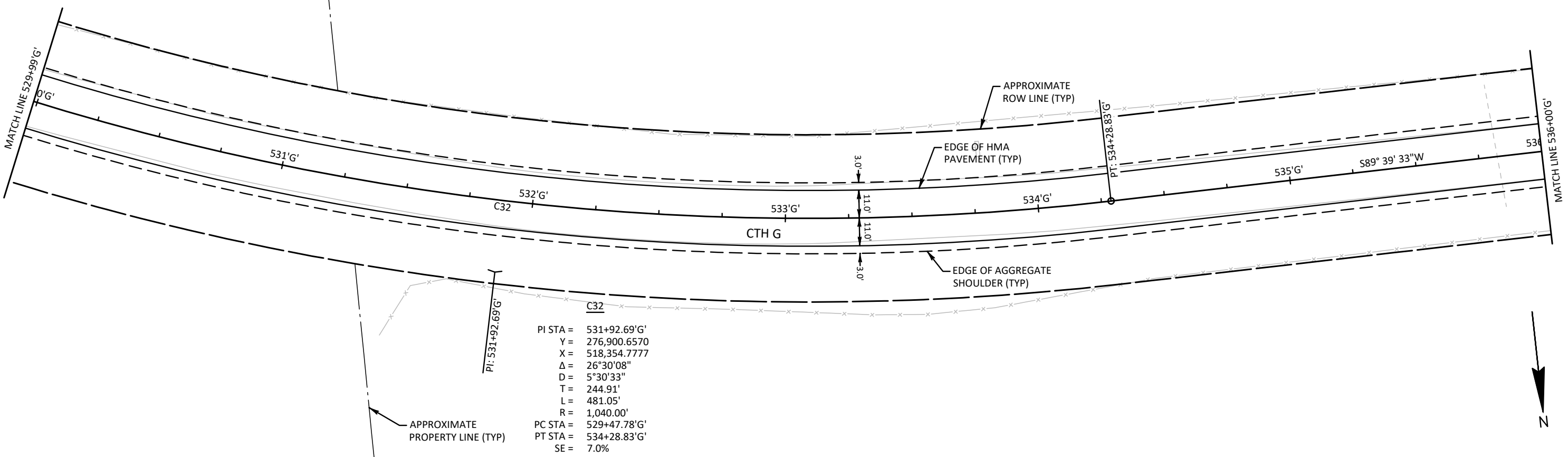


C31  
 PI STA = 524+79.76'G'  
 Y = 276,586.3292  
 X = 518,994.6684  
 Δ = 0°42'41"  
 D = 0°17'11"  
 T = 124.17'  
 L = 248.34'  
 R = 20,000.00'  
 PC STA = 523+55.59'G'  
 PT STA = 526+03.93'G'  
 SE = NC

PE STA 524+48 RT  
AGG PE

5

5



C32  
 PI STA = 531+92.69'G'  
 Y = 276,900.6570  
 X = 518,354.7777  
 Δ = 26°30'08"  
 D = 5°30'33"  
 T = 244.91'  
 L = 481.05'  
 R = 1,040.00'  
 PC STA = 529+47.78'G'  
 PT STA = 534+28.83'G'  
 SE = 7.0%

APPROXIMATE  
PROPERTY LINE (TYP)

PROJECT NO: 5908-00-74

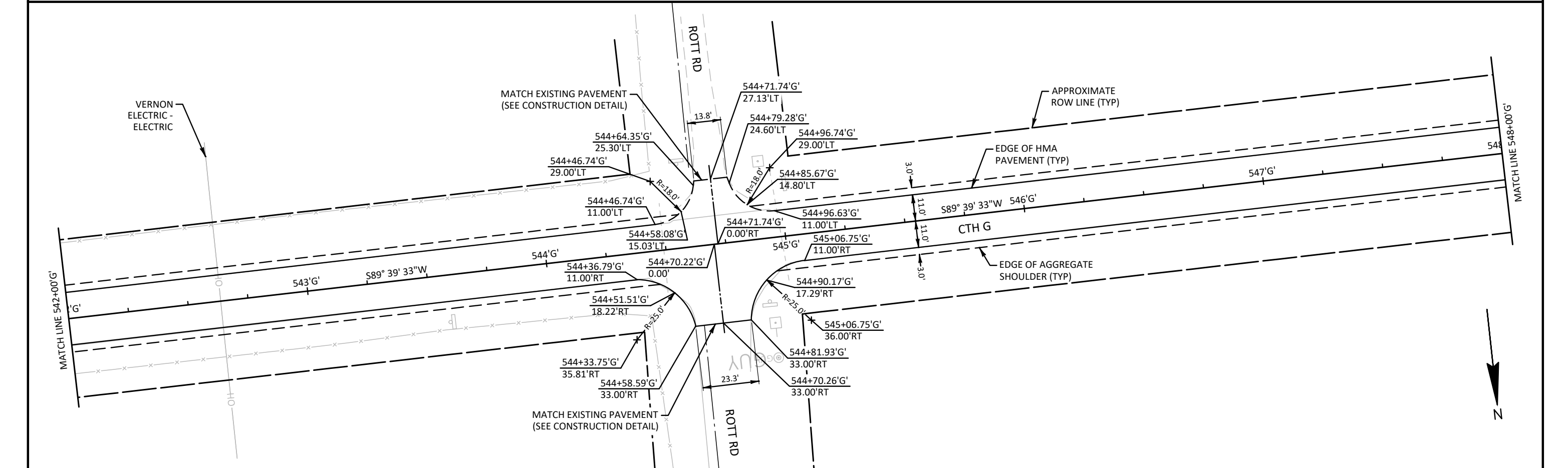
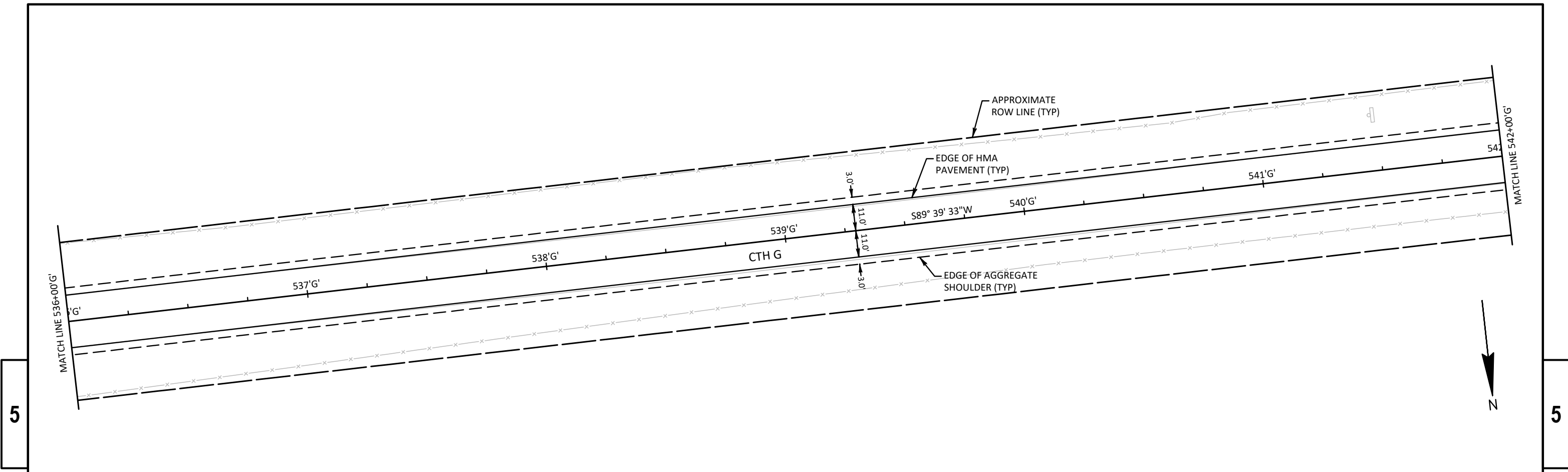
HWY: CTH G

COUNTY: SAUK

PLAN: CTH G

SHEET

E



PROJECT NO: 5908-00-74

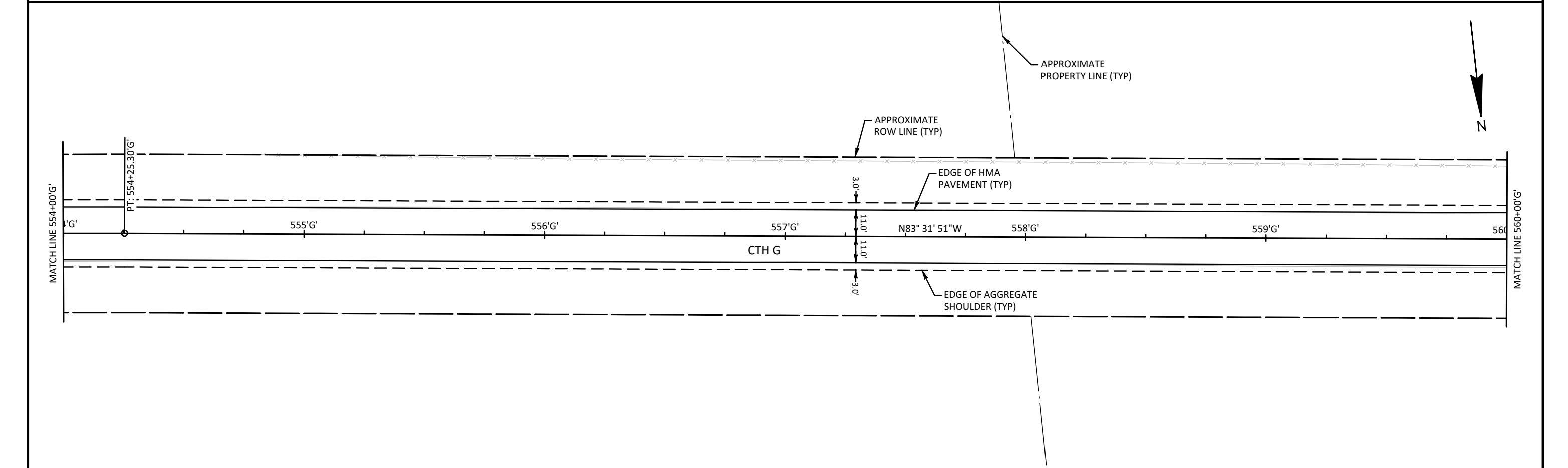
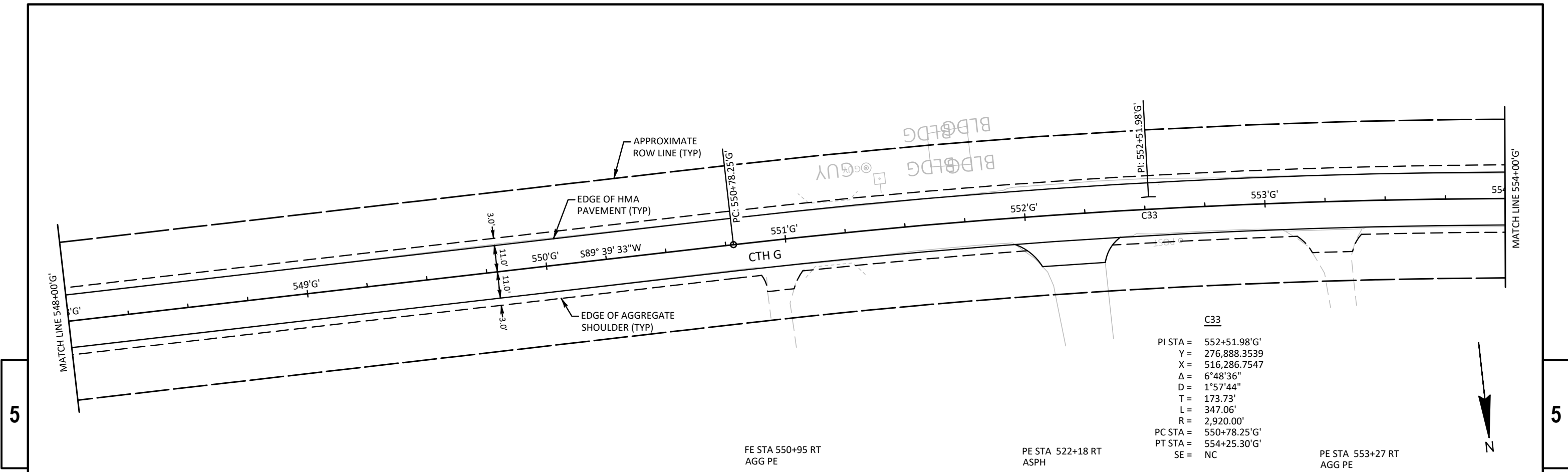
HWY: CTH G

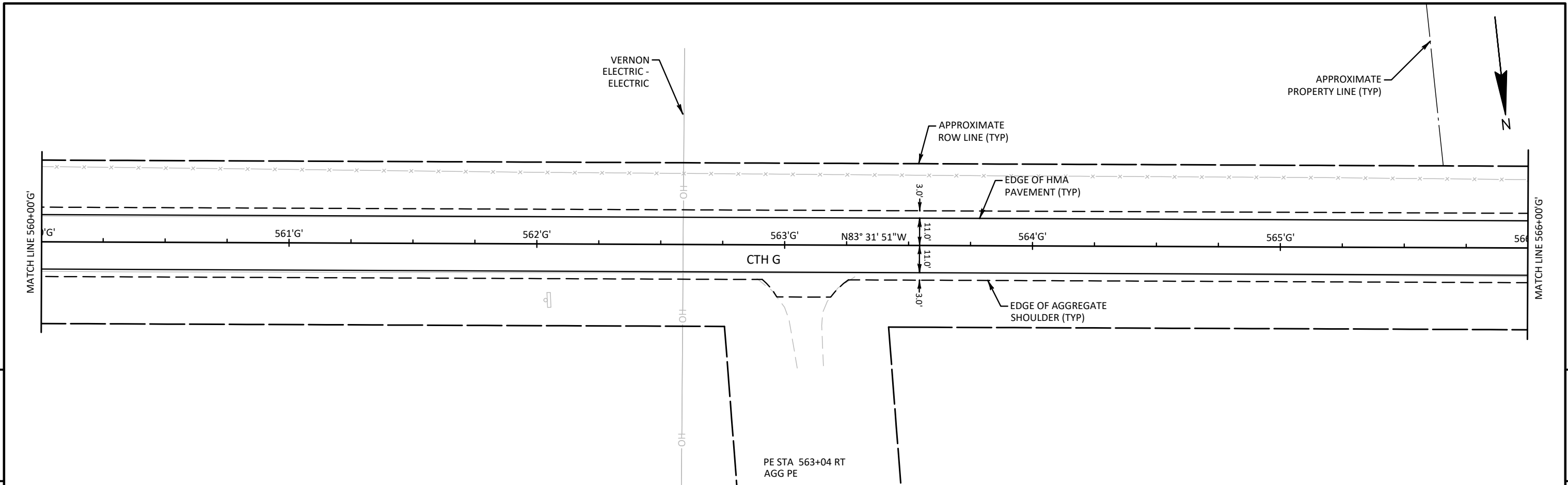
COUNTY: SAUK

PLAN: CTH G

SHEET

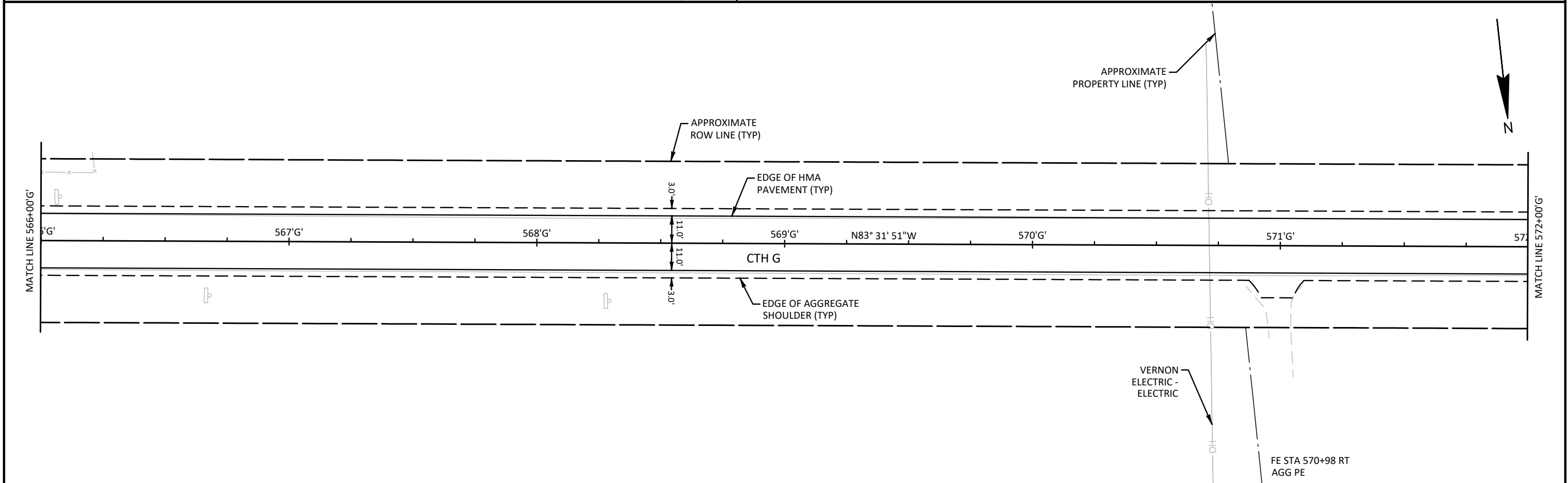
E



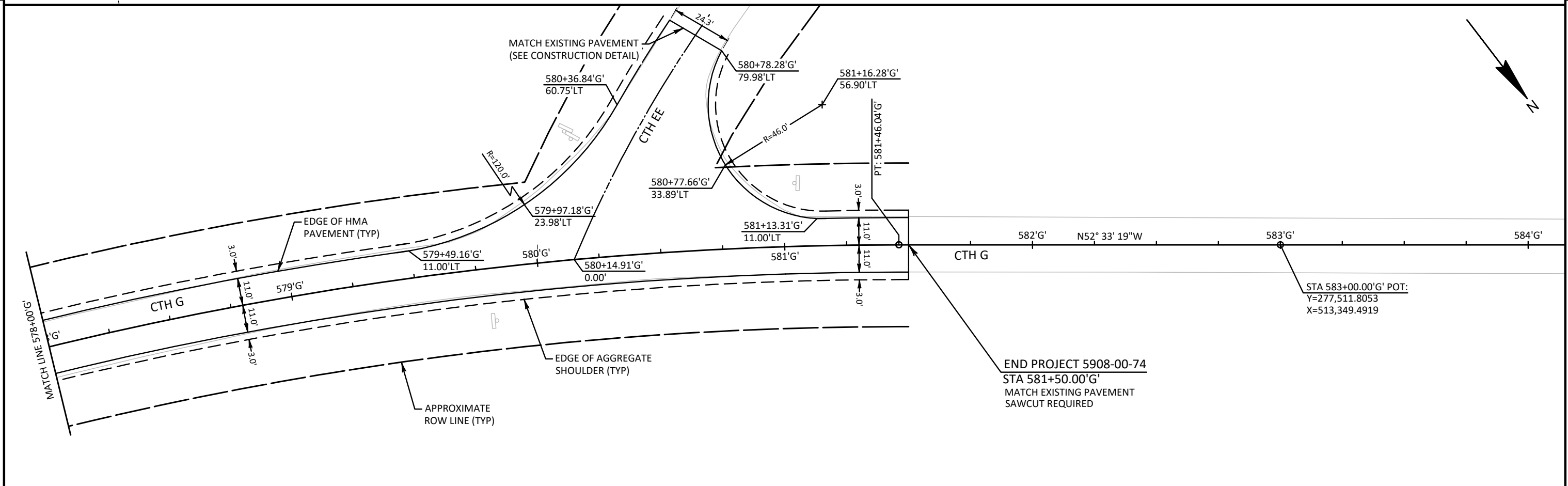
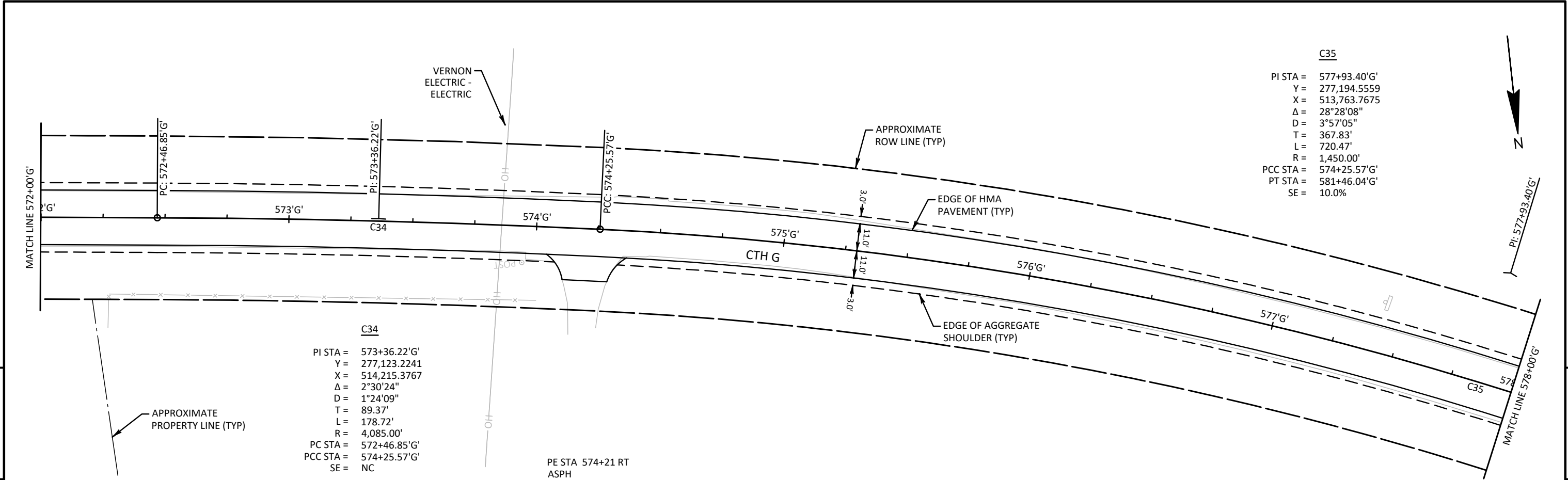


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PROJECT NO: 5908-00-74	HWY: CTH G	COUNTY: SAUK	PLAN: CTH G	SHEET	E
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PROJECT NO: 5908-00-74	HWY: CTH G	COUNTY: SAUK	PLAN: CTH G	SHEET	<b>E</b>
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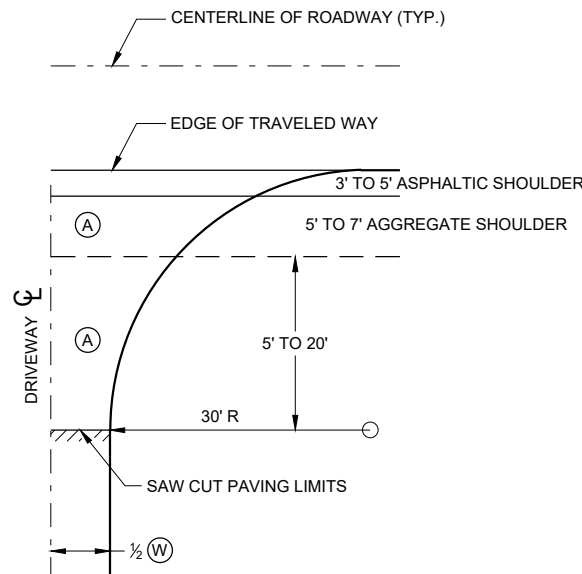


## Standard Detail Drawing List

08D22-01	DRIVEWAYS WITHOUT CURB & GUTTER RESURFACING PROJECTS RURAL
13C19-03	HMA LONGITUDINAL JOINTS
14B15-11A	STEEL PLATE BEAM GUARD, CLASS "A" INSTALLATION & ELEMENTS
14B15-11B	STEEL PLATE BEAM GUARD, CLASS "A" INSTALLATION & ELEMENTS
14B15-11C	STEEL PLATE BEAM GUARD, CLASS "A", INSTALLATION & ELEMENTS
14B29-01	SAFETY EDGE
15C02-08A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-08B	BARRICADES AND SIGNS FOR VARIOUS CLOSURES
15C08-21A	LONGITUDINAL MARKING (MAINLINE)
15C35-05A	PAVEMENT MARKING (INTERSECTIONS)

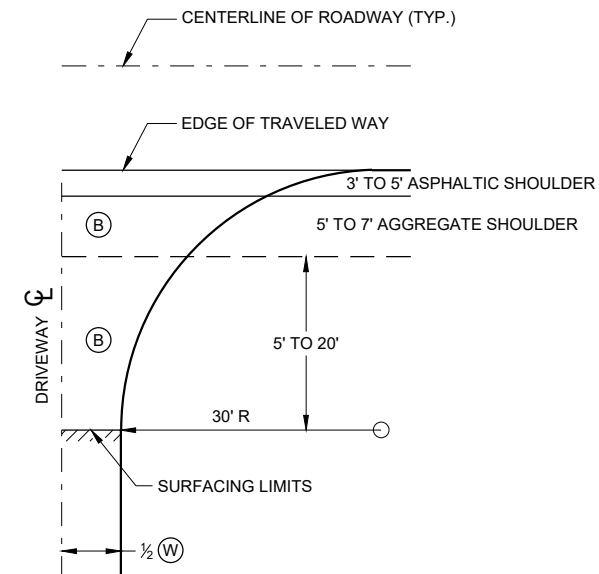
**GENERAL NOTES**

- ① DESIGN WILL DETERMINE FINAL DRIVEWAY ASPHALTIC THICKNESS BASED ON TYPE OF USAGE AND LOADINGS.

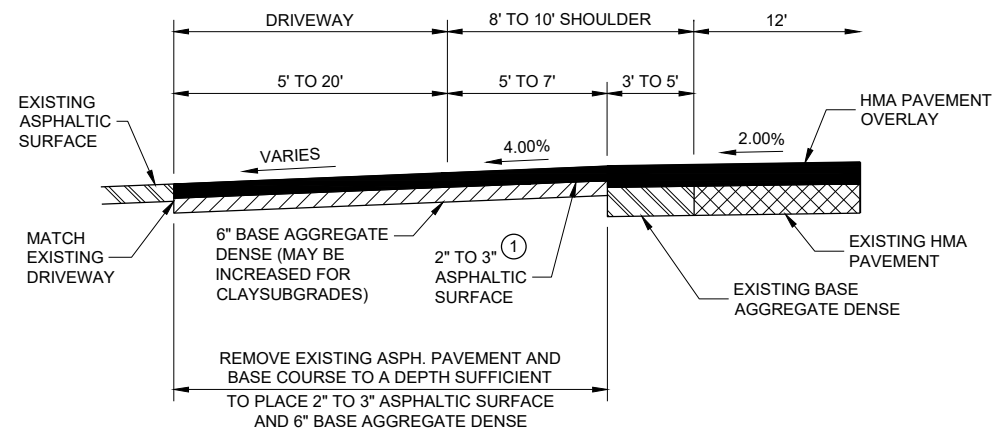


- Ⓐ : PAID FOR AS ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES. (TON)
- Ⓑ : PAID FOR AS BASE AGGREGATE DENSE 1 1/4" (TON)
- ⒲ : DRIVEWAY WIDTH 16' MIN. - 24' MAX.

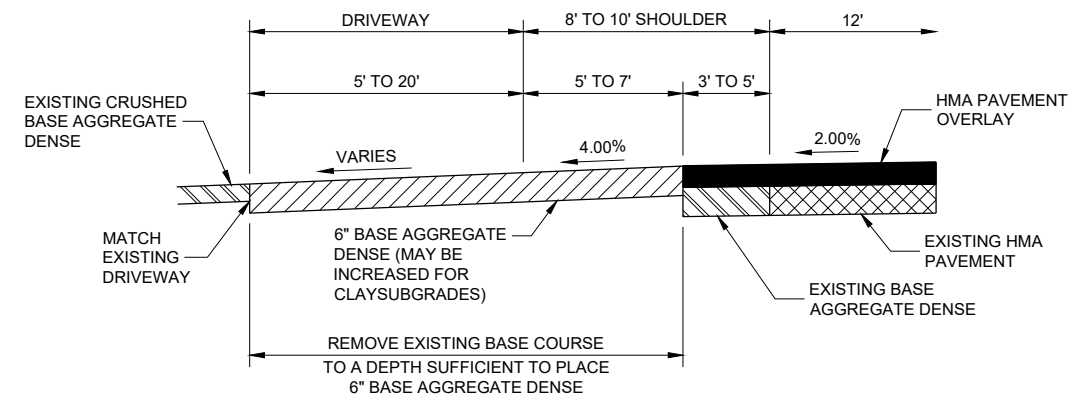
**PLAN VIEW  
HALF SECTION**



**PLAN VIEW  
HALF SECTION**



**PROFILE VIEW  
RURAL ENTRANCE  
WITH ASPHALTIC SURFACE  
RESURFACING PROJECTS**



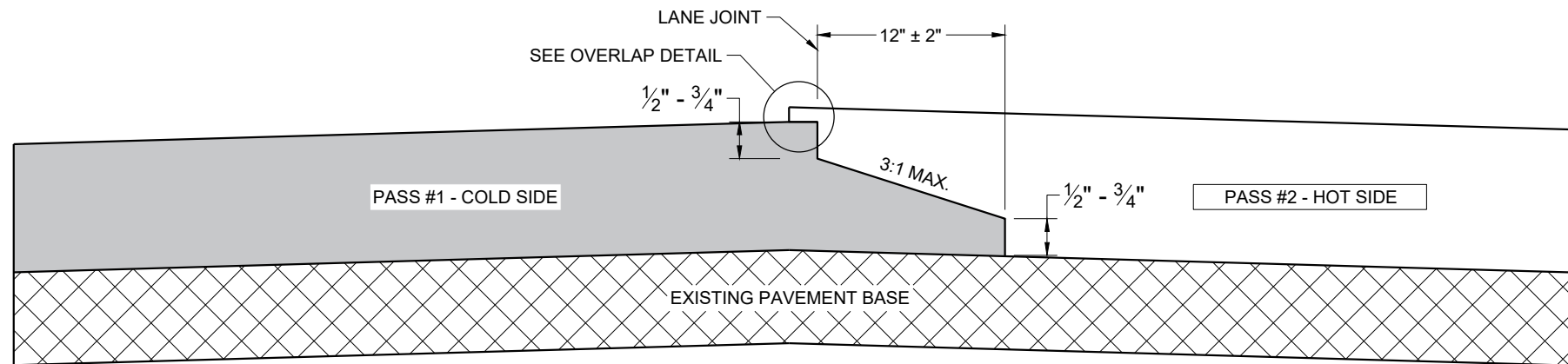
**PROFILE VIEW  
RURAL ENTRANCE  
WITH AGGREGATE SURFACE  
6" BASE AGGREGATE DENSE  
RESURFACING PROJECTS**

**DRIVEWAYS WITHOUT CURB  
AND GUTTER RESURFACING  
PROJECTS RURAL**

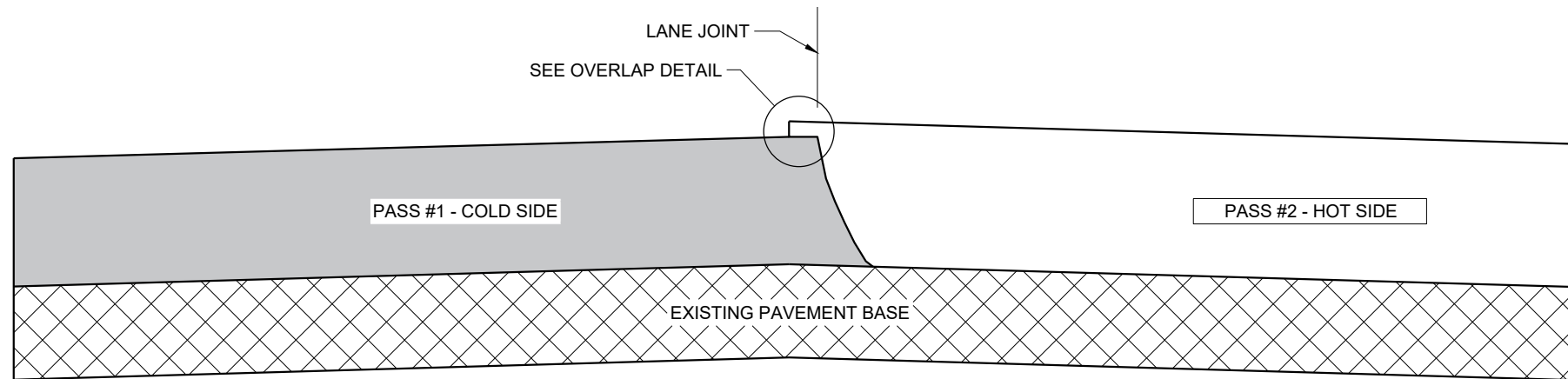
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
December 2016 /S/ Rodney Taylor  
DATE ROADWAY STANDARDS DEVELOPMENT  
ENGINEER

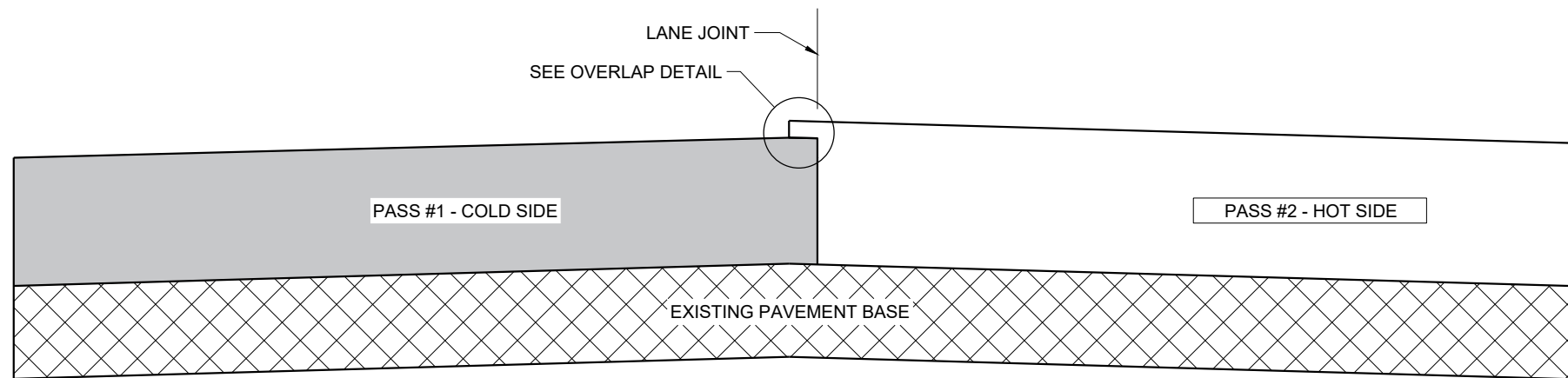
FHWA



**TYPICAL PAVEMENT CROSS SECTION  
NOTCHED WEDGE JOINT**



**TYPICAL PAVEMENT CROSS SECTION  
VERTICAL JOINT**



**TYPICAL PAVEMENT CROSS SECTION  
VERTICAL JOINT (MILLED)**

**GENERAL NOTES**

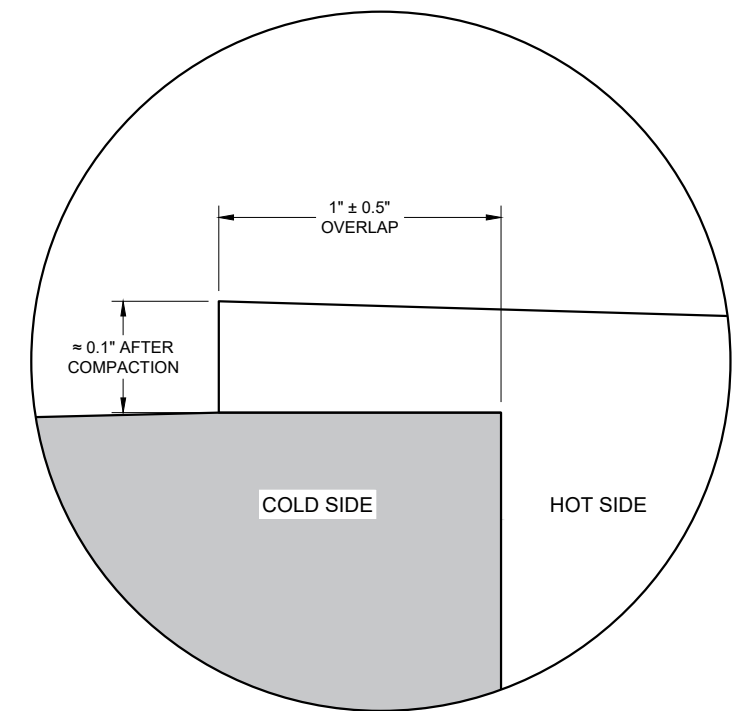
IN ADDITION TO THE DETAILS PROVIDED IN THIS DRAWING, CONFORM TO STANDARD SPECIFICATION 450.3.2.8 FOR WHEN A NOTCHED WEDGE JOINT IS REQUIRED AND FOR GENERAL JOINT CONSTRUCTION REQUIREMENTS.

FOR ALL LONGITUDINAL JOINTS, ENSURE THE PAVER SCREED OVERLAPS THE PREVIOUSLY PLACED PAVEMENT BY  $1" \pm 0.5"$  AND THE HOT SIDE OF THE JOINT REMAINS HIGHER THAN THE COLD SIDE BY APPROXIMATELY 0.1" AFTER FINAL COMPACTION. (IT WILL BE FLUSH WHEN PAVING IN ECHELON.)

ONLY REMOVE THE LONGITUDINAL NOTCHED WEDGE JOINT FOR SMA PAVEMENT OR AS DIRECTED BY THE ENGINEER TO ADDRESS SPECIFIC LENGTHS OF JOINT DAMAGED BY TRAFFIC.

WHEN MILLING BACK OR REMOVING ANY LONGITUDINAL JOINT, LIMIT THE MATERIAL REMOVED TO 2" FROM THE TOP NOTCH OR FROM THE VERTICAL JOINT EDGE ON THE COLD SIDE OF THE JOINT.

USE LONGITUDINAL MILLED JOINT AS PLANS SHOW OR THE AS THE ENGINEER DIRECTS.



**OVERLAP DETAIL (TYPICAL)**

6

6

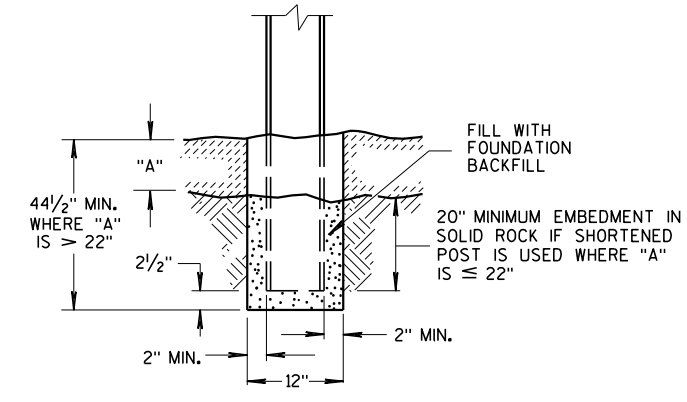
SDD 13C19 - 03

SDD 13C19 - 03

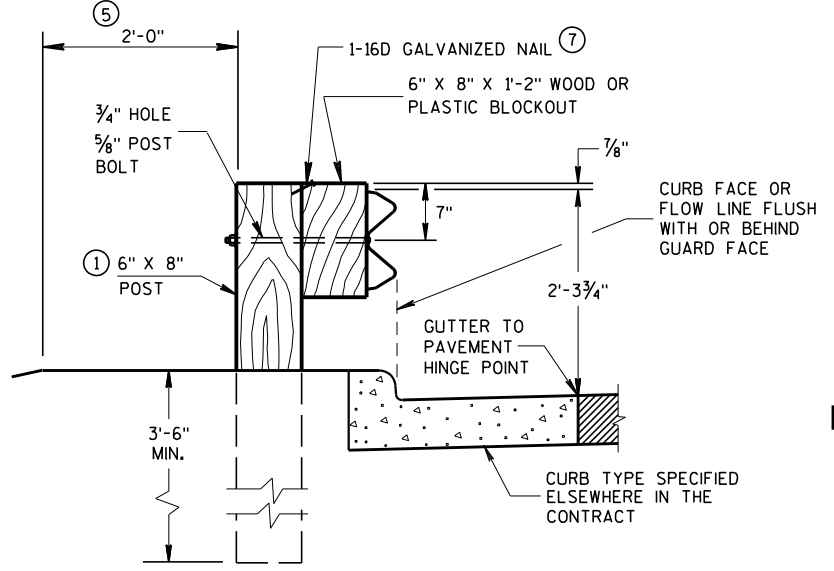
<b>HMA LONGITUDINAL JOINTS</b>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED November 2020 DATE	/S/ Steven Hefel HMA PAVEMENT ENGINEER
FHWA	

**GENERAL NOTES**

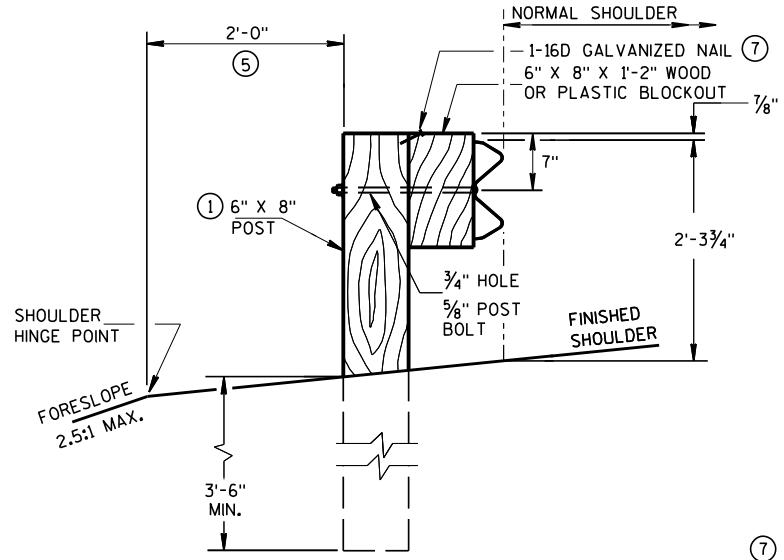
- ① W6 X 9 OR W6 X 8.5 STEEL POSTS AND NOTCHED PLASTIC BLOCKOUTS ARE ACCEPTABLE ALTERNATIVES FOR 6" X 8" WOOD POSTS WITH WOOD OR PLASTIC BLOCKOUTS. USE APPROVED NOTCHED PLASTIC BLOCKOUTS WITH STEEL POSTS. APPROVED PLASTIC BLOCKOUT DESIGNS MAY VARY FROM THIS TYPICAL DETAIL WHEN USED IN CONJUNCTION WITH STEEL POSTS. DO NOT MIX STEEL POSTS AND WOOD POSTS IN A SINGLE INSTALLATION.
  - ② USE STRUCTURAL STEEL POSTS CONFORMING TO ASTM A 36. GALVANIZED POSTS ACCORDING TO AASHTO M 111. EITHER SET THE POSTS IN DRILLED HOLES OR DRIVE TO GRADE. REMOVE MUSHROOMING CAUSED BY DRIVING AND REPAIR DAMAGED SPELTER COATING ON GALVANIZED POSTS.
  - ③ INSTALL STEEL POSTS WITH HOLES ON APPROACHING TRAFFIC SIDE.
  - ④ USE EITHER WOOD OR APPROVED PLASTIC BLOCKOUTS ON WOOD POSTS.
  - ⑤ IF THE DISTANCE FROM BACK OF POST TO SHOULDER HINGE POINT IS LESS THAN 2 FEET INSTALL LONGER POST AT HALF POST SPACING, W BEAM (LHW).
  - ⑥ IF ROCK IS ENCOUNTERED DURING EXCAVATION, THE ENGINEER MAY APPROVE USING A 12 INCH DIAMETER POST HOLE EXTENDING 20 INCHES DEEP INTO THE ROCK. PLACE GRANULAR MATERIAL IN THE BOTTOM OF THE HOLE APPROXIMATELY 2 1/2 INCHES DEEP. CUT THE POSTS TO LENGTH AND PLACE IN THE HOLE. BACKFILL WITH MATERIAL EXCAVATED FROM THE HOLE AND COMPACT ADEQUATELY.
  - ⑦ WHEN USING STEEL POSTS AND WOOD BLOCKOUTS INSTALL FOUR 16D GALVANIZED NAILS. INSTALL NAILS AT THE BACK CORNERS OF THE BLOCK AND BEND THE NAILS OVER THE FLANGE OF THE STEEL POST.
- INSTALL BEAM GUARD SECTIONS AND ALL NECESSARY HARDWARE ACCORDING TO THE APPLICABLE PLAN AND CURRENT STANDARD AND SUPPLEMENTAL SPECIFICATIONS. ALL DIMENSIONS ARE SUBJECT TO MANUFACTURER'S TOLERANCES EXCEPT WHERE ALLOWABLE TOLERANCES ARE SHOWN.



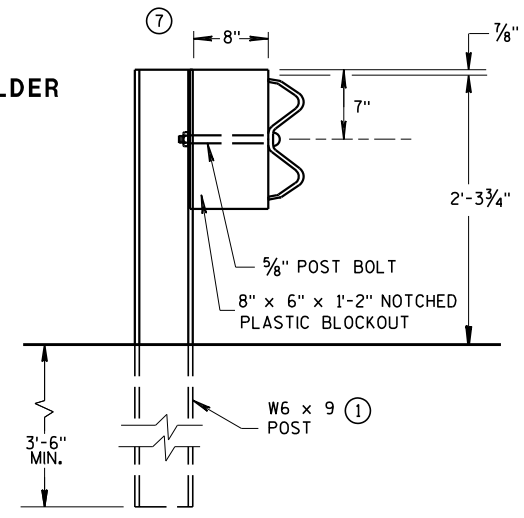
**END VIEW SETTING STEEL OR WOOD POST IN ROCK** ⑥



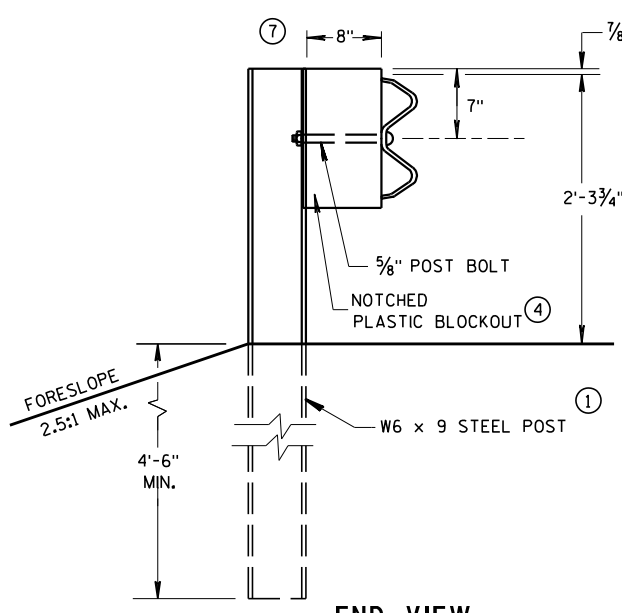
**END VIEW LOCATED ALONG A CURBED ROADWAY**



**END VIEW LOCATED ALONG A ROADWAY SHOULDER STANDARD INSTALLATION**

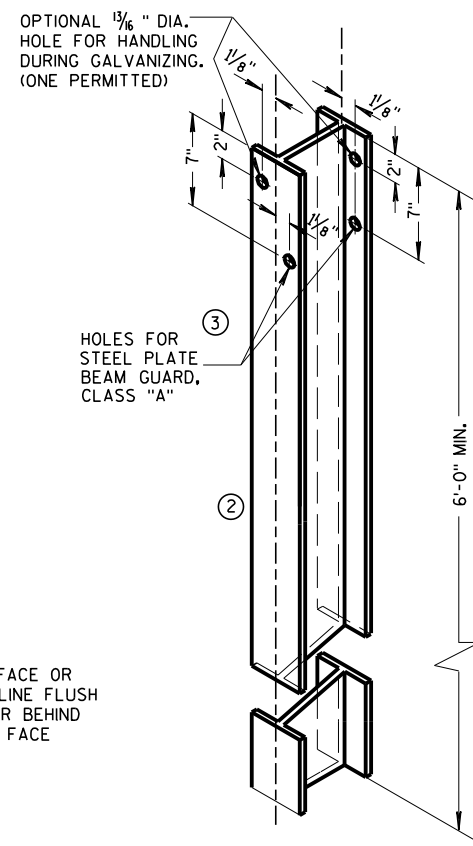


**END VIEW STEEL POST & NOTCHED PLASTIC BLOCKOUT ALTERNATIVE STANDARD INSTALLATION**

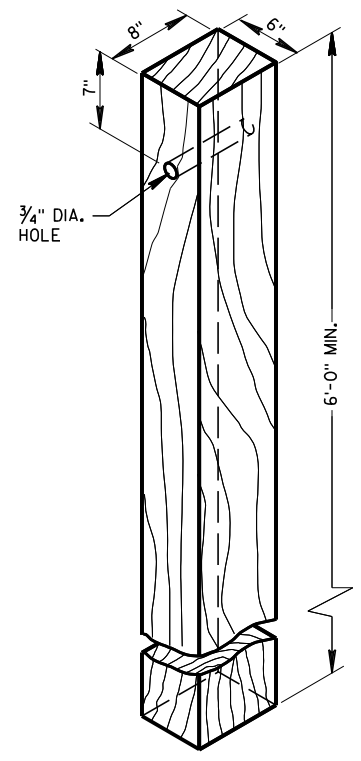


**END VIEW LONGER POST AT HALF POST SPACING W BEAM (LHW)**

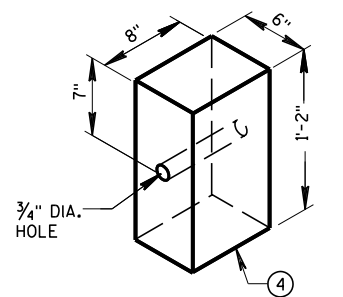
**TYPICAL INSTALLATION OF STEEL PLATE BEAM GUARD**



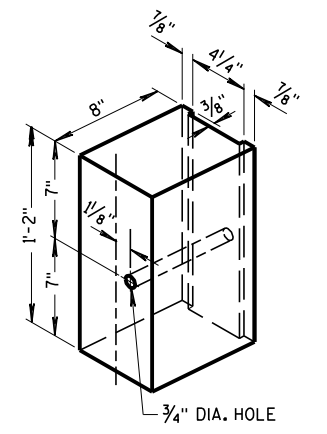
**STEEL POST & HOLE PUNCHING DETAIL (W6 X 9)** ①  
ALL HOLES 13/16" DIAMETER EXCEPT AS NOTED



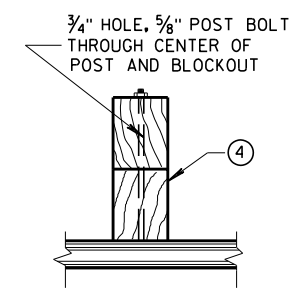
**WOOD POST (6" X 8") NOMINAL**



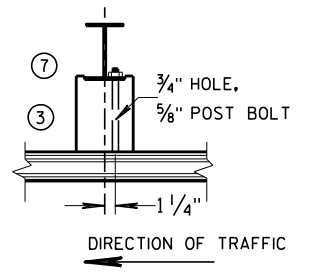
**WOOD OR PLASTIC BLOCKOUT FOR WOOD POSTS**



**TYPICAL NOTCHED PLASTIC BLOCKOUT FOR STEEL POSTS** ①



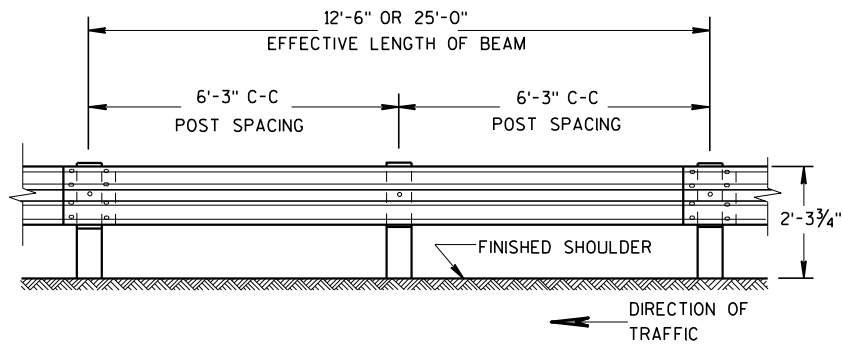
**PLAN VIEW WOOD POST, BLOCKOUT & BEAM**



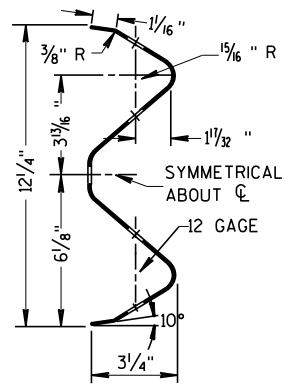
**PLAN VIEW STEEL POST, NOTCHED PLASTIC BLOCKOUT & BEAM**

**STEEL PLATE BEAM GUARD, CLASS "A" INSTALLATION & ELEMENTS**

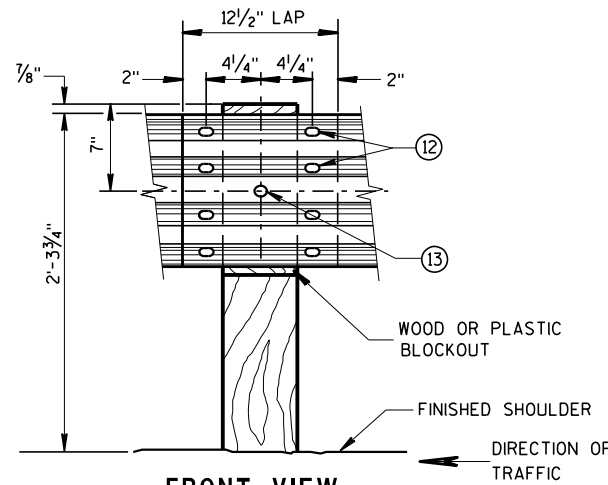
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION



**FRONT VIEW  
POST SPACING STANDARD INSTALLATION**



**SECTION THRU W BEAM**

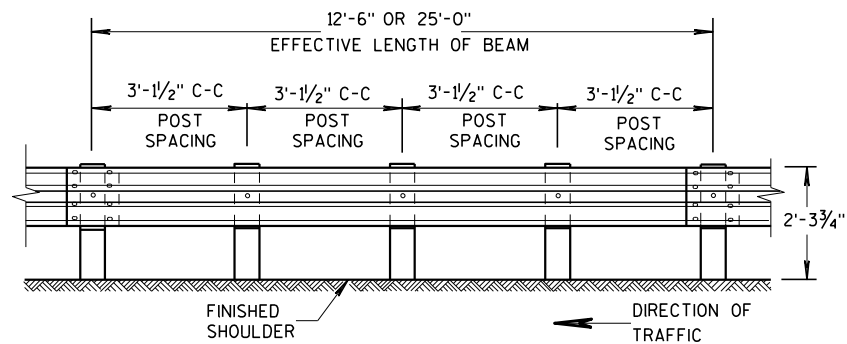


**FRONT VIEW  
BEAM SPLICE AT WOOD POST  
AND POST MOUNTING DETAIL**

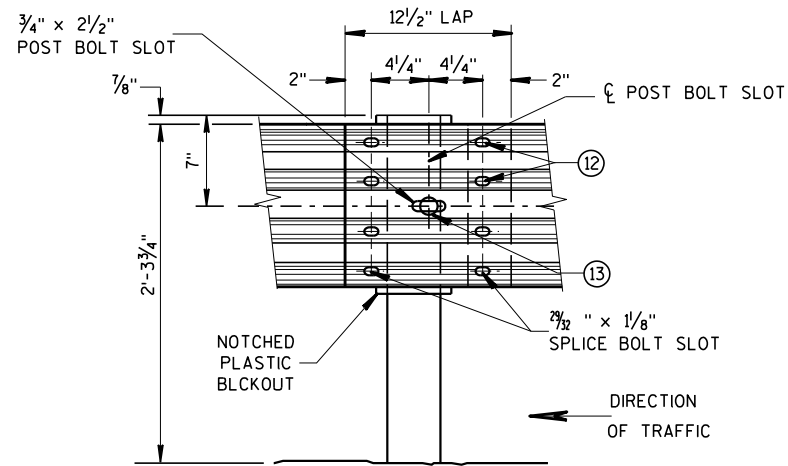
**GENERAL NOTES**

FURNISH GUARDRAIL DEFLECTORS FROM APPROVED PRODUCTS LIST.

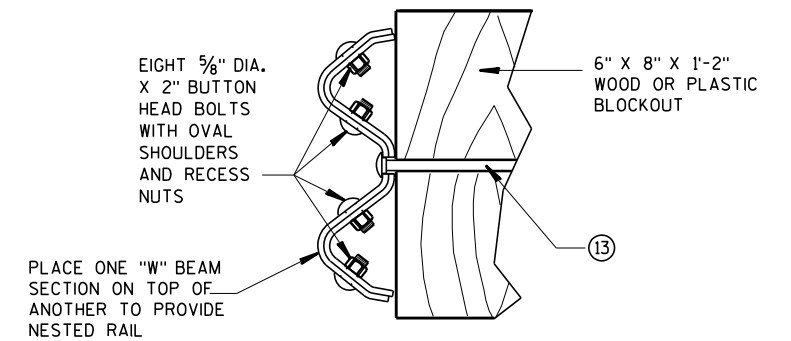
- ⑨ DO NOT INSTALL REFLECTORS ON THE FIRST 50 FEET OF THE APPROACH END OF THE ENERGY ABSORBING TERMINA. START REFLECTORS AT POST #9 AND SPACE EVENLY EVERY 100 FEET (MAX.) TO THE END OF GUARDRAIL RUN, USING A MINIMUM OF 3 REFLECTORS.
- ⑫ 8 - 5/8"  $\phi$  X 2" BUTTON HEAD BOLTS WITH OVAL SHOULDERS & RECESS NUTS.
- ⑬ 5/8" DIA. BUTTON HEAD BOLT AND RECESS NUT WITH 5/8" DIA. F844 FLAT WASHER UNDER NUT.



**FRONT VIEW  
POST SPACING FOR LONGER POST  
AT HALF POST SPACING W BEAM (LHW)**

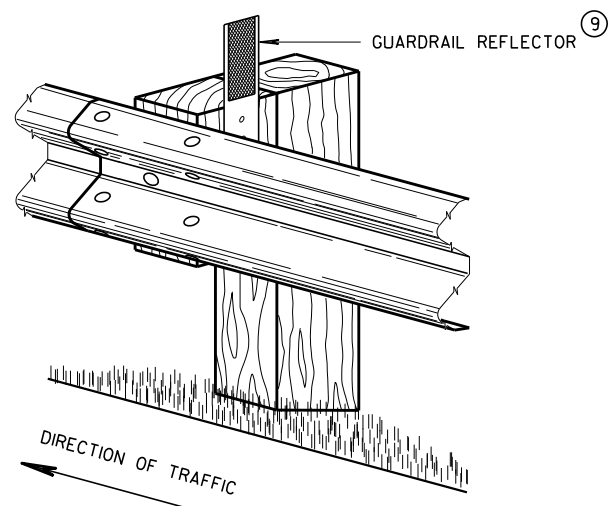


**FRONT VIEW  
BEAM SPLICE AT STEEL POST  
TYPICAL SPLICING DETAILS  
OF STEEL PLATE BEAM GUARD**

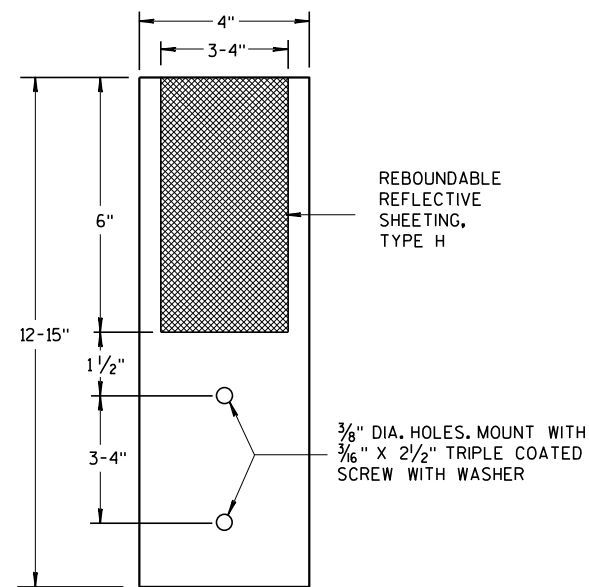


**NESTED W BEAM (NW)**  
USE ALL OTHER STANDARD BEAM GUARD DETAILS FOR  
CONSTRUCTING NESTED W BEAM (NW)

\* USE DOUBLE SIDED WHITE GUARDRAIL REFLECTORS ON ROADWAYS WITH BI-DIRECTIONAL TRAFFIC (NO MEDIAN). USE SINGLE SIDED WHITE (RIGHT SIDE) AND SINGLE SIDED YELLOW (LEFT SIDE) ON ROADWAYS WITH MEDIAN SEPARATION.



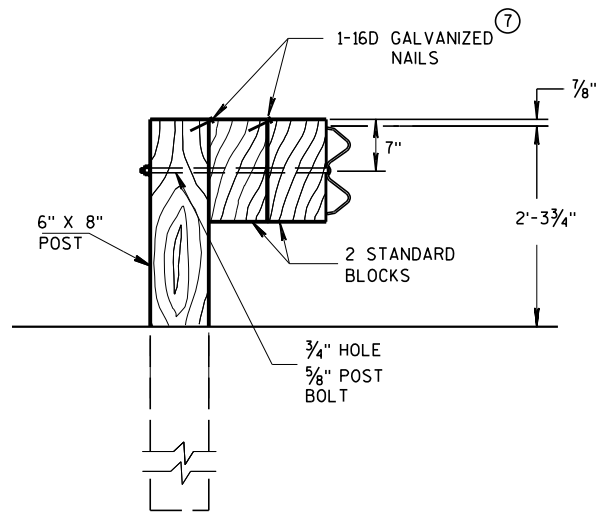
**4" X 12" GUARDRAIL REFLECTOR DETAIL  
AND TYPICAL INSTALLATION \***



**4" x 12" GUARDRAIL REFLECTOR**

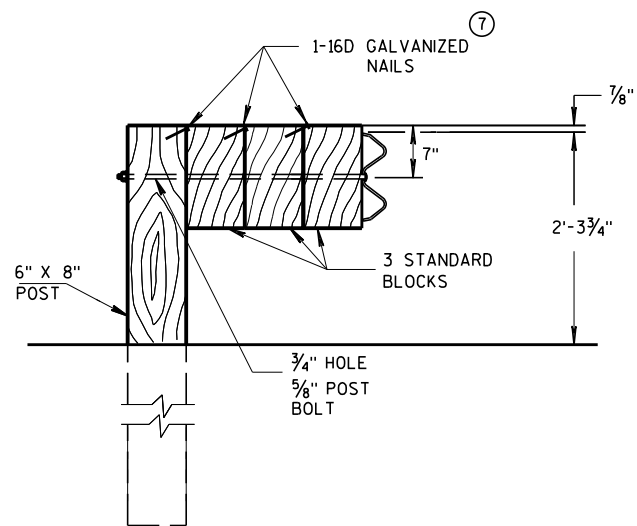
**STEEL PLATE BEAM GUARD,  
CLASS "A",  
INSTALLATION & ELEMENTS**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION



**DETAIL FOR DOUBLE BLOCKS**

THE NUMBER OF DOUBLE BLOCK POSTS WITHIN A BARRIER RUN IS UNLIMITED

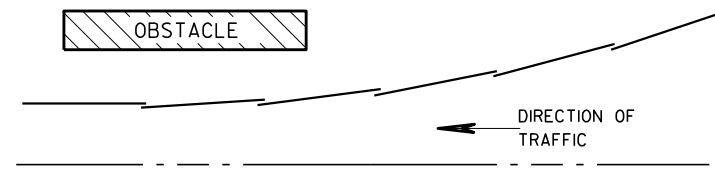


**DETAIL FOR TRIPLE BLOCKS**

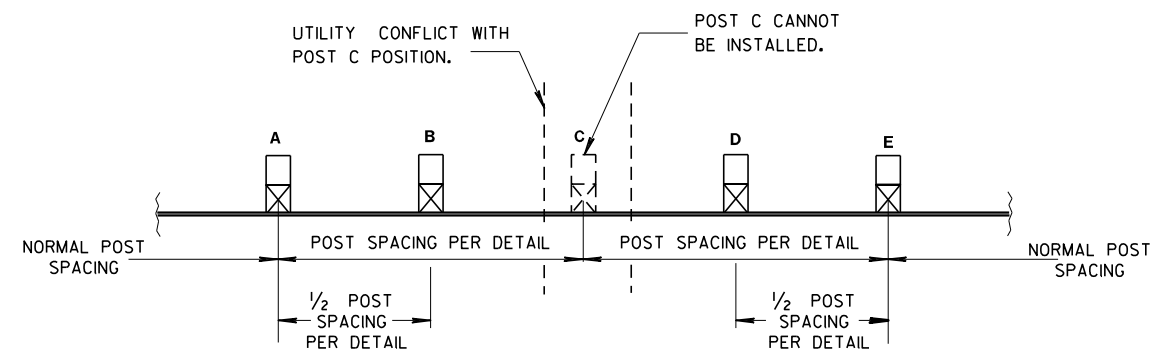
TRIPLE BLOCK DETAIL IS LIMITED TO ONE LOCATION WITHIN A BEAM GUARD RUN.

NOTES: USE DOUBLE OR TRIPLE BLOCKS WHEN UNDERGROUND OBSTACLES PREVENT THE POST FROM BEING INSTALLED.

DO NOT USE EXTRA BLOCKOUTS IF IT CAUSES THE POST TO BE DRIVEN BEYOND SHOULDER HINGE POINT OR CAUSES A FIXED OBJECT TO BE WITHIN THE DEFLECTION DISTANCE OF THE BARRIER.

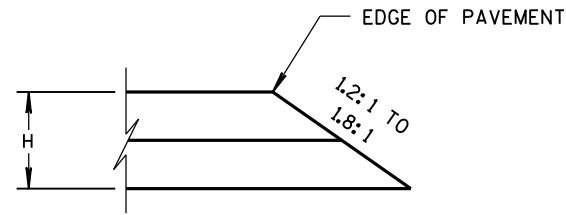


**PLAN VIEW  
BEAM LAPPING DETAIL**

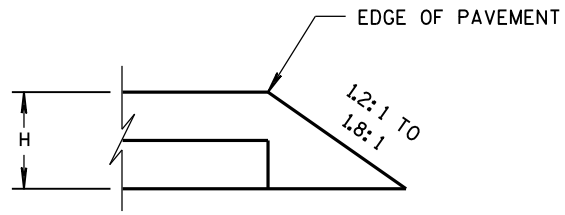


**POST DRIVING FOR CONTINUOUS  
UNDERGROUND OBSTRUCTION**

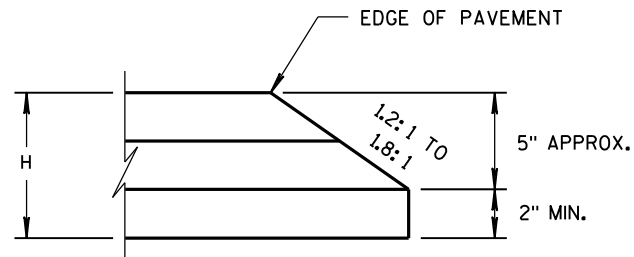
<b>STEEL PLATE BEAM GUARD, CLASS "A", INSTALLATION &amp; ELEMENTS</b>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED June 2017	/s/ Rodney Taylor
DATE	ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR
FHWA	



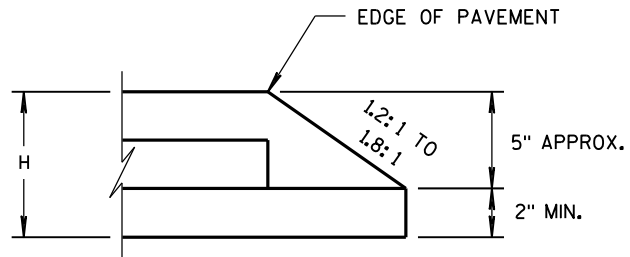
CONSTRUCTED WITH FINAL TWO LAYERS  
FOR H 5" OR LESS



CONSTRUCTED WITH FINAL LAYER  
FOR H 5" OR LESS

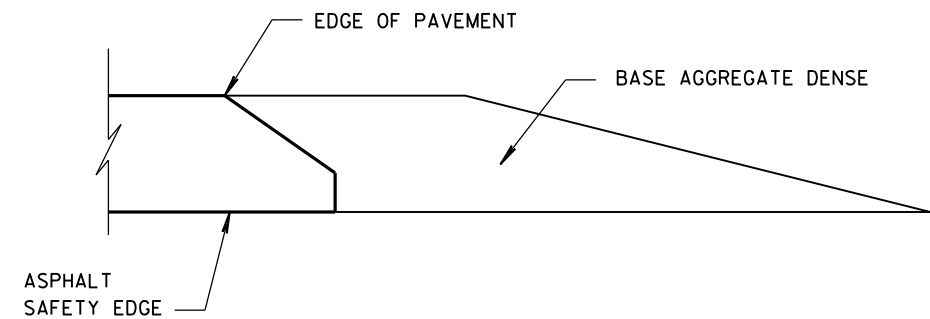


CONSTRUCTED WITH FINAL TWO LAYERS  
FOR H GREATER THAN 5"



CONSTRUCTED WITH FINAL LAYER  
FOR H GREATER THAN 5"

HMA PAVEMENT AND HMA OVERLAYS



FINISHED SHOULDER AGGREGATE PLACEMENT

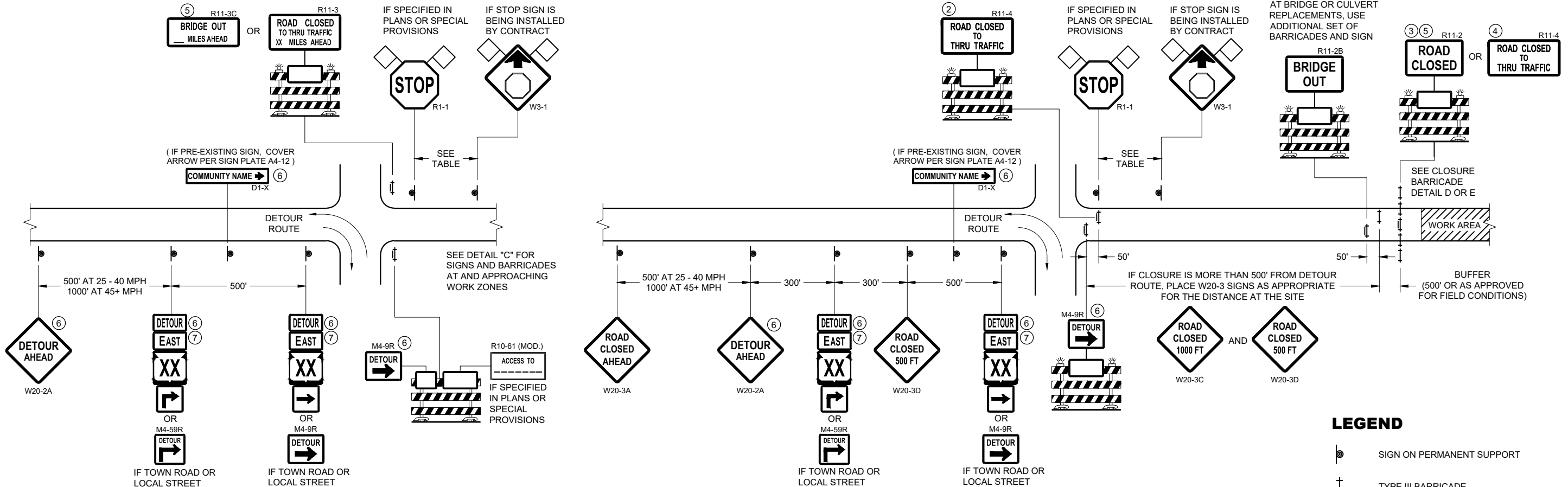
6

6

S.D.D. 14 B 29-1

S.D.D. 14 B 29-1

SAFETY EDGE <sub>SM</sub>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED DATE	/s/ Jerry H. Zogg ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA	



**DETAIL A  
MAINLINE CLOSURE WITH POSTED DETOUR**

WORK ZONE GREATER THAN OR EQUAL TO 1/2 MILE FROM  
DETOUR ROUTE ( 1000 FEET IF URBAN )

**DETAIL B  
MAINLINE CLOSURE WITH POSTED DETOUR**

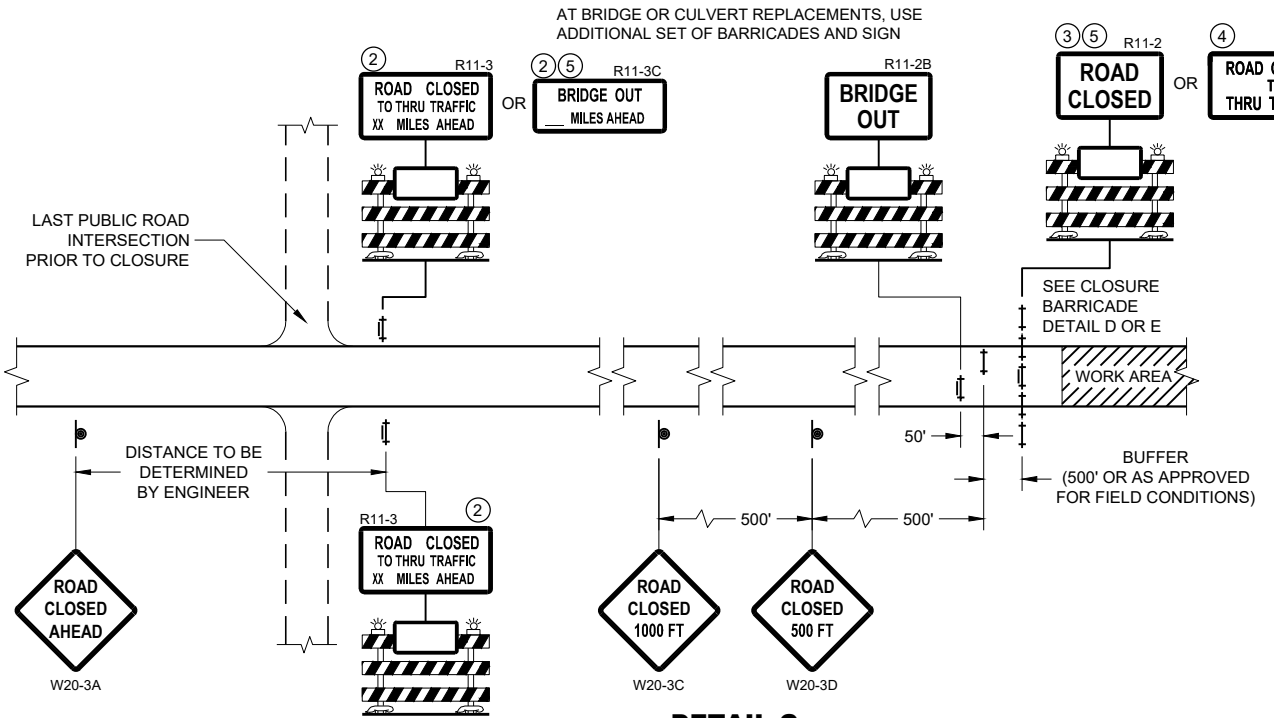
WORK ZONE LESS THAN 1/2 MILE FROM  
DETOUR ROUTE ( 1000 FEET IF URBAN )

**LEGEND**

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
- FLAGS, 16" X 16" MIN. (ORANGE)

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

- M4 - 8
- M3 - X
- M1 - 4
- M1 - 6
- M1 - 5A
- M05 - 1
- M06 - 1



**DETAIL C  
MAINLINE CLOSURE, NO POSTED DETOUR**

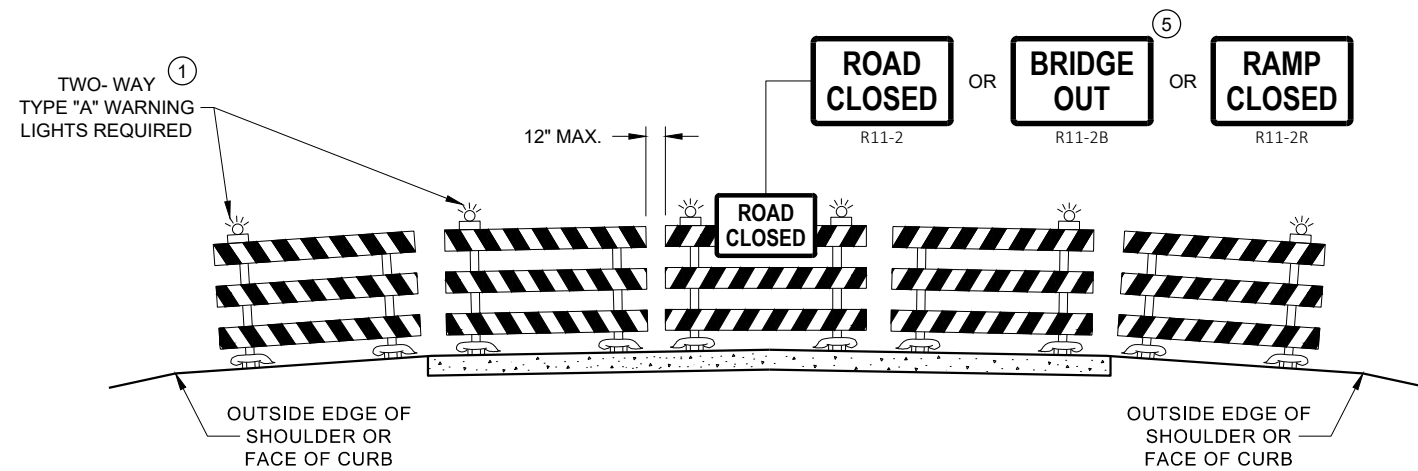
SEE SDD 15C2-SHEET "b"  
FOR GENERAL NOTES  
AND FOOTNOTES ① THROUGH ⑦

**BARRICADES AND SIGNS  
FOR MAINLINE CLOSURES**

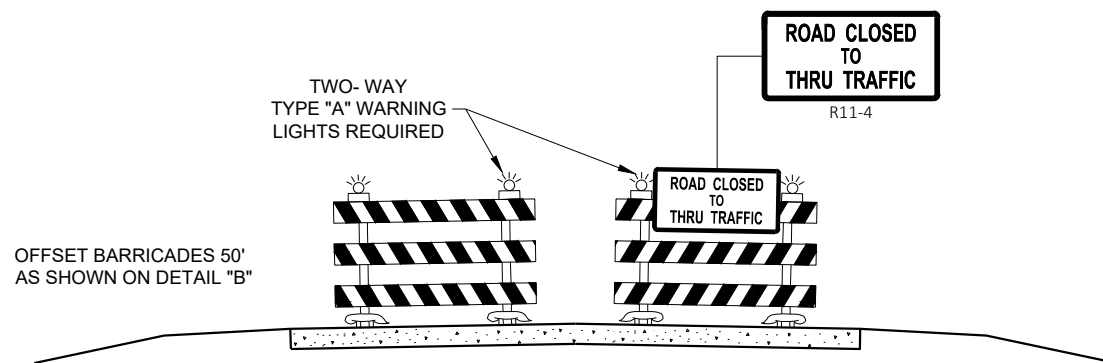
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
February 2020 /S/ Andrew Heidtke  
DATE DATE WORK ZONE ENGINEER  
FHWA





**DETAIL D  
ROAD CLOSURE BARRICADE DETAIL  
APPROACH VIEW**



**DETAIL E  
LANE CLOSURE BARRICADE DETAIL  
APPROACH VIEW**

SEE SDD 15C2 - SHEET "a" FOR LEGEND

**GENERAL NOTES**

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW - INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11 - 2, R11 - 3, M4 - 9, R11 - 4, AND R10 - 61 SIGNS PLACED ON THE BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE RAIL OR BOTTOM RAILS.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

- R11 - 2 SHALL BE 48" X 30"
- R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60" X 30"
- M4 - 9 SHALL BE 30" X 24"
- M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1 - 1 SHALL BE 36" X 36"

- ① TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT SPACING).
- ② THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.
- ③ FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".
- ④ FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".
- ⑤ FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.
- ⑥ INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.
- ⑦ "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

**BARRICADES AND SIGNS  
FOR  
VARIOUS CLOSURES**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
February 2020 /S/ Andrew Heidtke  
DATE WORK ZONE ENGINEER




FHWA

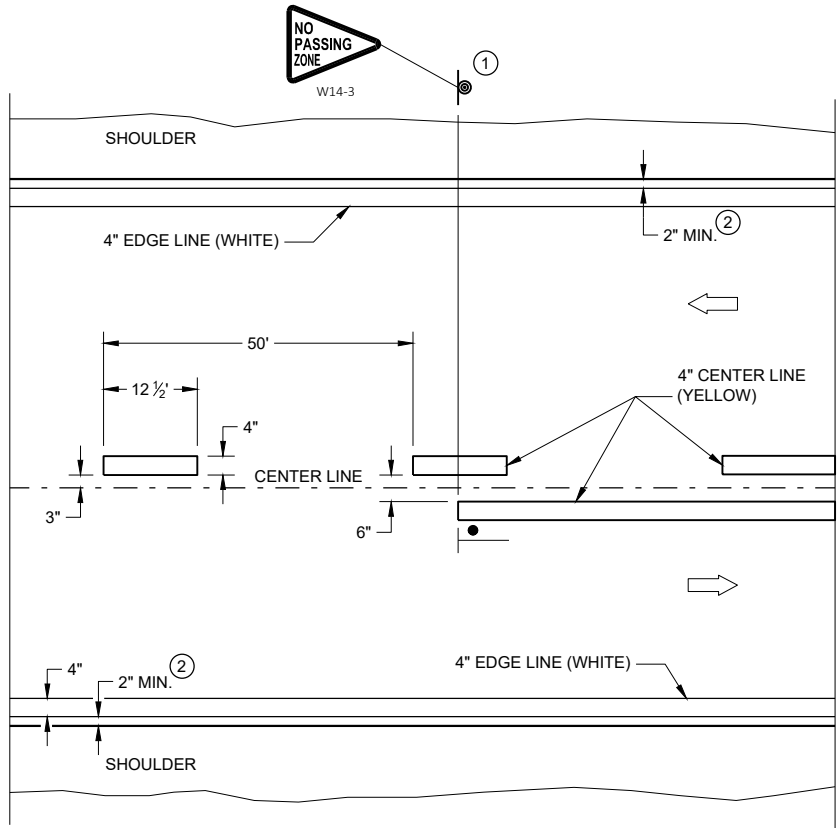
**GENERAL NOTES**

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

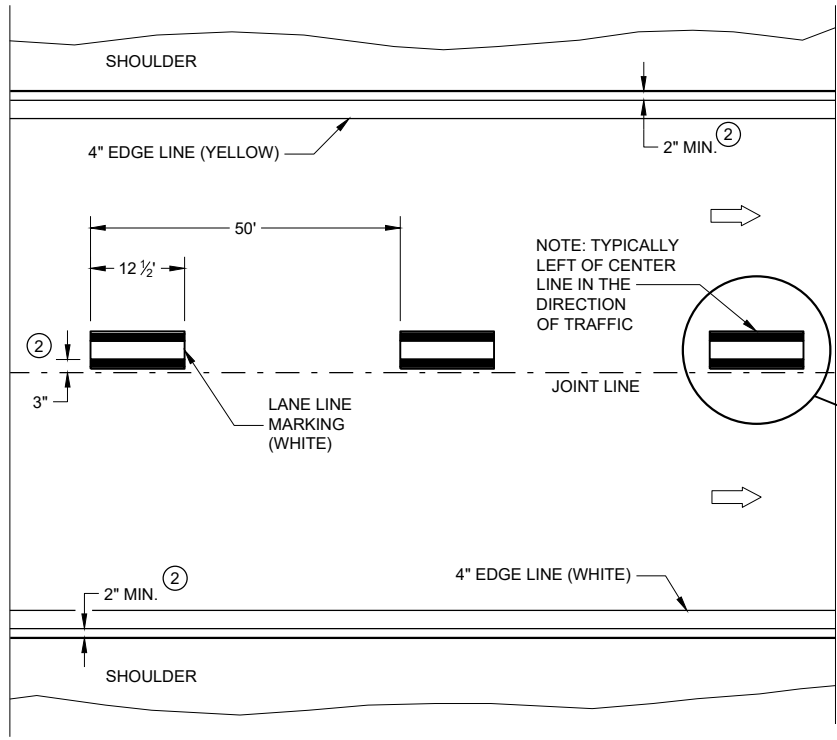
- ① LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING
- ② MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

**LEGEND**

-  "T" MARKING
-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC

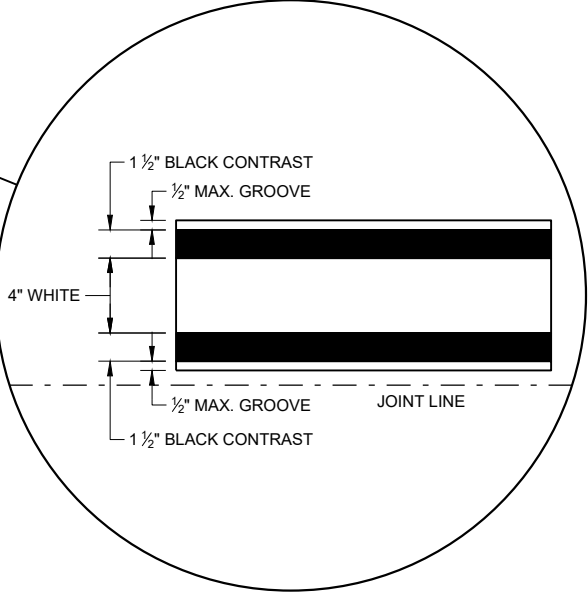


**TWO WAY TRAFFIC**



**ONE WAY TRAFFIC**

**PERMANENT PAVEMENT MARKING**



**PERMANENT LONGITUDINAL PAVEMENT MARKINGS**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
DATE: May 2022 /S/ Jeannie Silver  
STATEWIDE SIGNING AND MARKING ENGINEER

FHWA

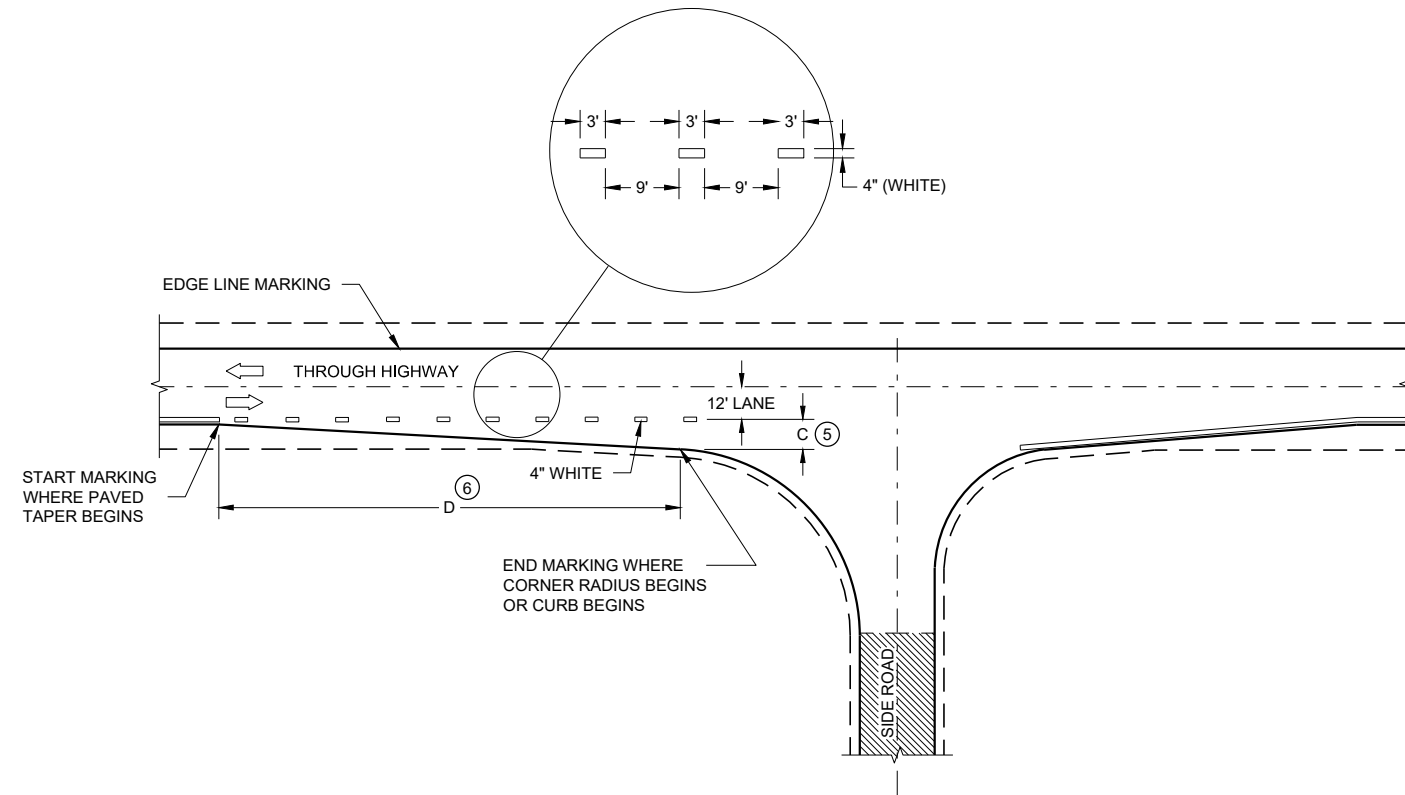
**GENERAL NOTES**

OMIT EDGE LINES THROUGH INTERSECTIONS. CONTINUE EDGE LINES THROUGH DRIVEWAYS.

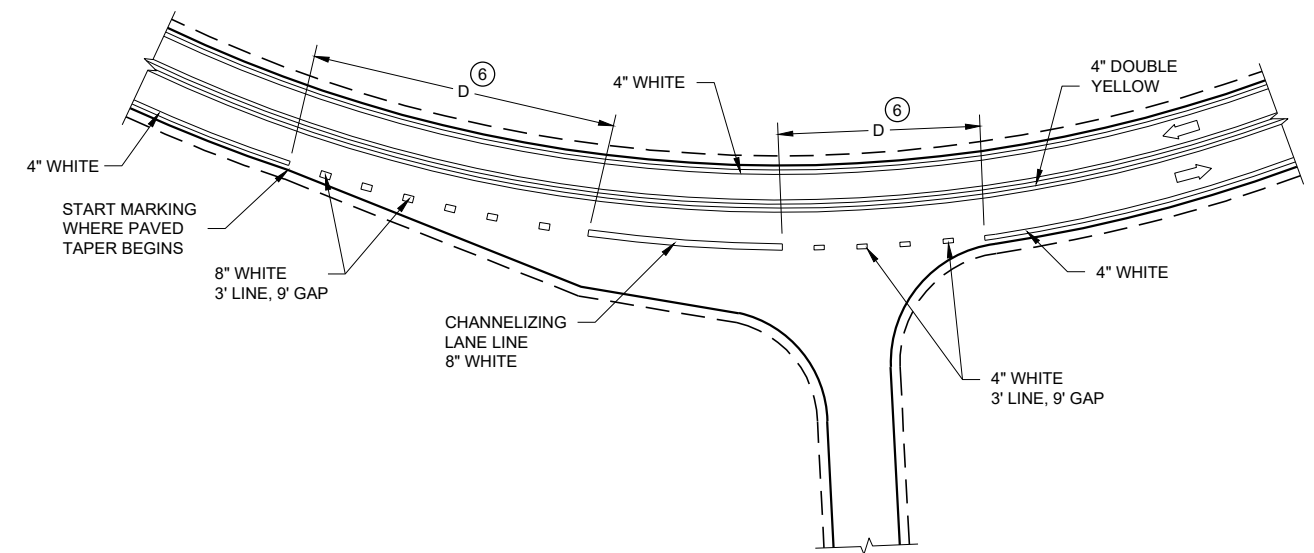
- ① WHEN DISTANCE "A" IS LESS THAN 250 FEET, OMIT LANE LINE.
- ② WHEN DISTANCE "B" IS LESS THAN 100 FEET, OMIT CHANNELIZING LANE LINE.
- ③ BARRIER LINE ENDS AT SIDE ROAD PAVEMENT / SURFACE EDGE EXTENSION.
- ④ BARRIER LINE STARTS 500 FEET PRIOR TO THE BYPASS TAPER.
- ⑤ WHEN DISTANCE "C" IS LESS THAN 4 FEET, OMIT DOTTED EXTENSION.
- ⑥ WHEN DISTANCE "D" IS LESS THAN 50 FEET, OMIT DOTTED EXTENSION.

**LEGEND**

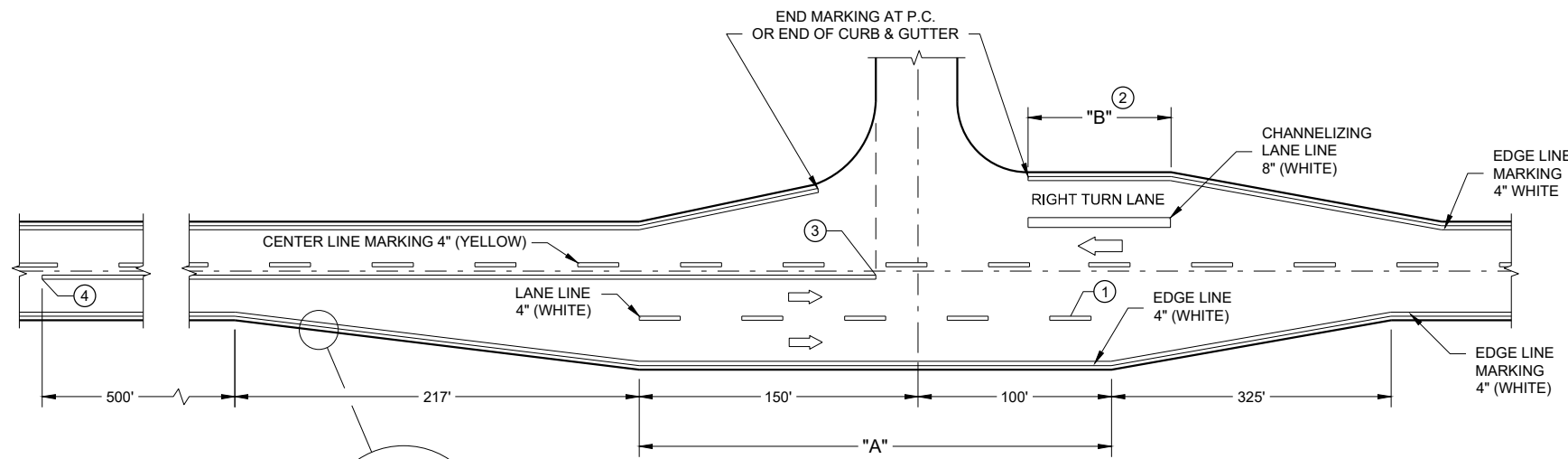
➔ DIRECTION OF TRAVEL



**MINOR INTERSECTION**

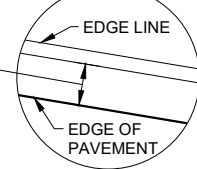


**INTERSECTION ON OUTSIDE OF CURVE**



**MAJOR INTERSECTIONS  
(INTERSECTION WITH FULL RIGHT TURN LANE OR BYPASS LANE)**

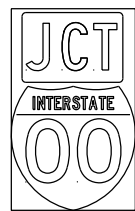
BYPASS LANE PAVED SHOULDER WIDTH (AS SHOWN ELSEWHERE IN PLANS) - PLUS 2 INCHES



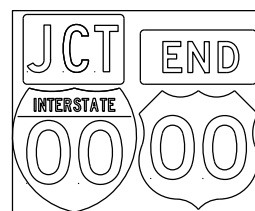
**PAVEMENT MARKING  
(INTERSECTIONS)**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

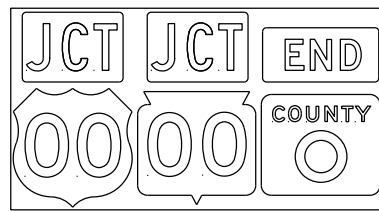
# TYPICAL ASSEMBLIES



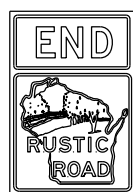
J1-1



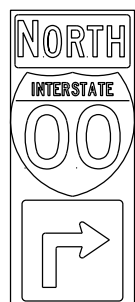
J1-2



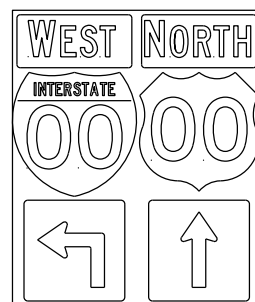
J1-3



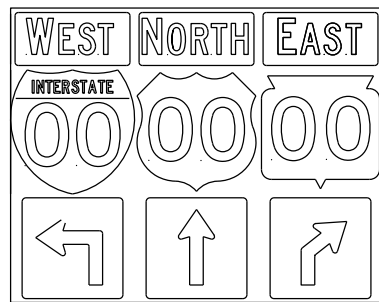
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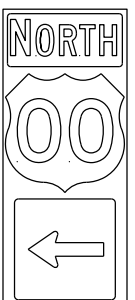
J2-1



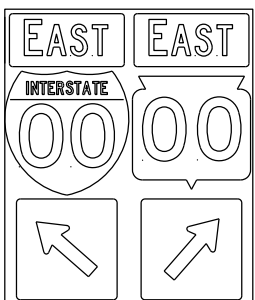
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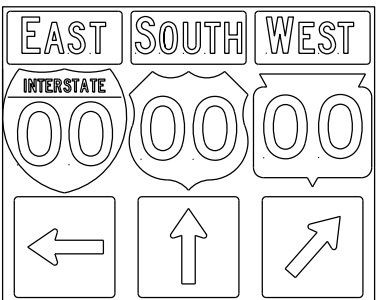
J2-3



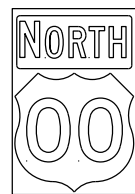
J3-1



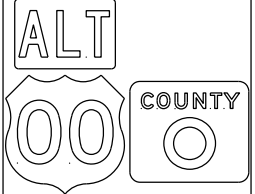
J3-2



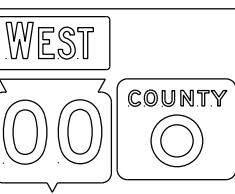
J3-3



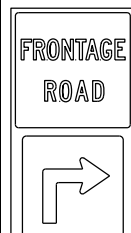
J4-1



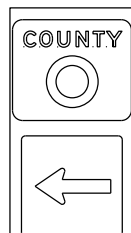
J4-2



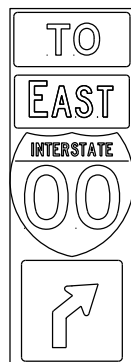
J4-2



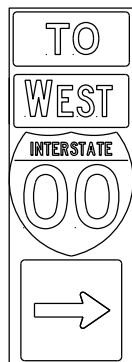
J12-1



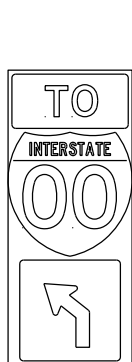
J13-1



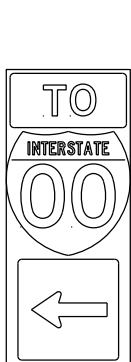
J32-1



J33-1



J22-1



J23-1



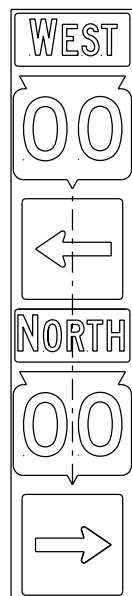
JR13-1



JR23-1

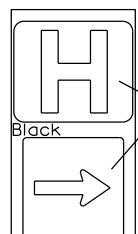


JR99-1



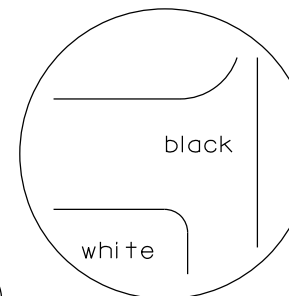
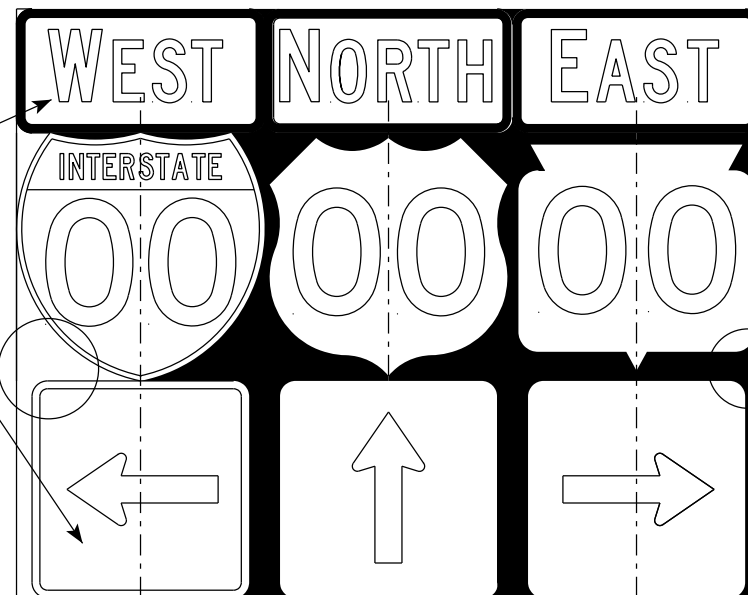
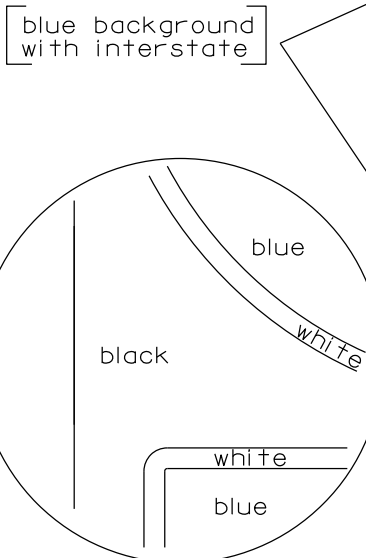
JV

(Typical Vertical J-Assembly See Note 10 and 11)



JH-1

Blue Background



black background

## NOTES

- Signs are Type II - Type H Reflective
- Color:
  - Background - Black Non-reflective
  - Message - see Note 5
- Message Series - See Note 5
- Corners shall be square or rounded if base material is plywood. If base material is metal the corners shall be rounded.
- The colors and message spacing on each marker shall be according to the applicable route marker panel specifications.
- Certain marker heads require the component pieces to be the same color. As an example, all the components used with an MI-1 Interstate marker shall be blue.
- Single panel j-assemblies shall only be used with route marker shields that are same size. If the route marker shields are different size use multiple piece component.
- Route assemblies that have 24 inch route shields and have dimensions greater than 48 inches (both vertical and horizontal) shall have one horizontal splice between the arrows and route shields. Vertical splices shall not be used on route assemblies with a horizontal dimension of 144 inches or less. The contractor shall not use more than one vertical joint per sign and the joint shall be between route shields.
- Route assemblies that have 36 inch shields and have dimensions greater than 48 inches (both vertical and horizontal) shall have two horizontal splices. One horizontal splice shall be between the cardinal direction and route shields and the other horizontal splice shall be between the arrows and route shields. Vertical splices shall not be used on route assemblies with a horizontal dimension of 144 or less. The contractor shall not use more than one vertical joint per sign and the joint shall be between route shields.
- All Vertical J Assemblies are given a Sign Code of JV
- For JV Assemblies that have a mixture of Interstate and non Interstate shields, arrows and cardinals shall be white on blue.

### ROUTE MARKERS & COMPONENTS IN TYPICAL ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

*Matthew R. Rauch*  
for State Traffic Engineer

DATE 3/18/21

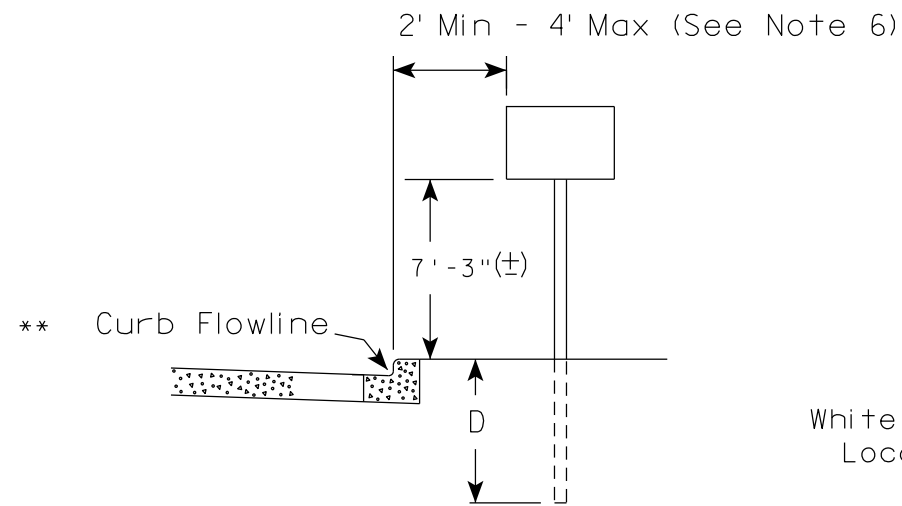
PLATE NO. A2-1S.9

PROJECT NO:

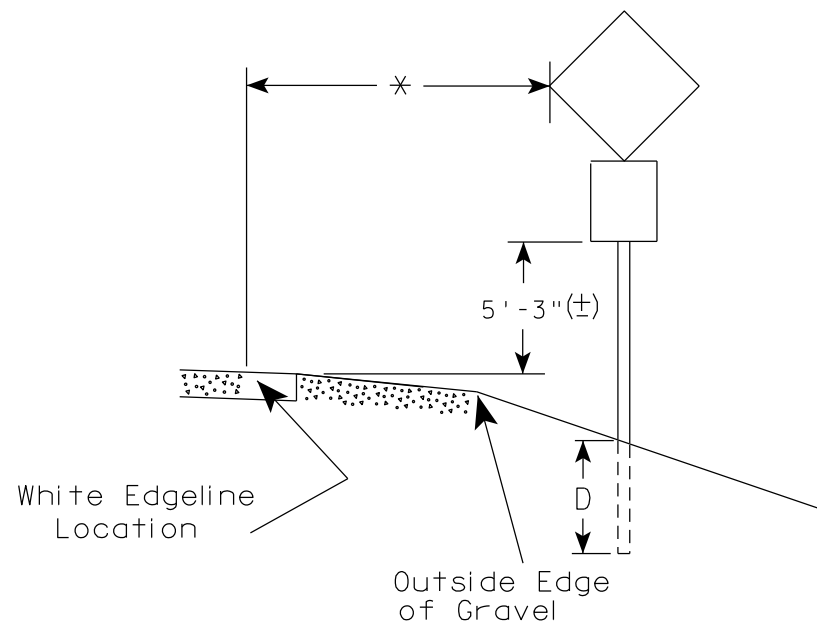
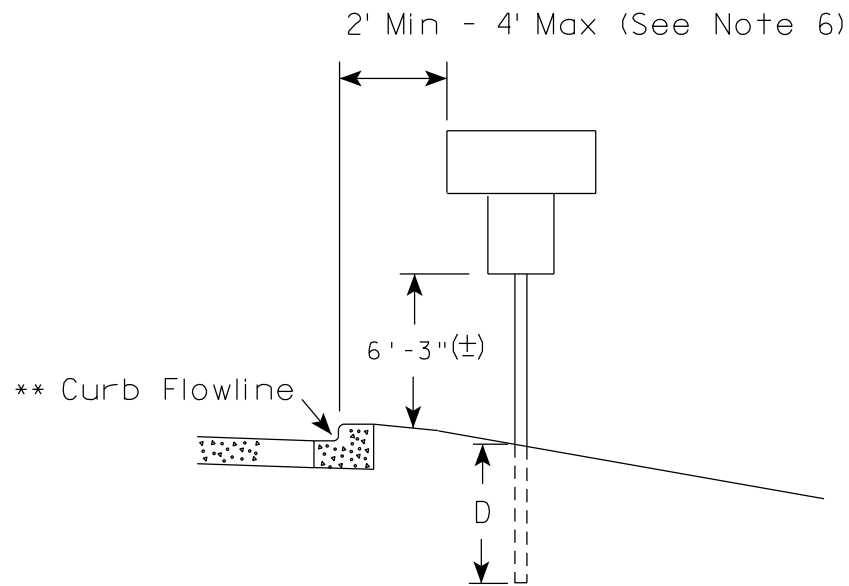
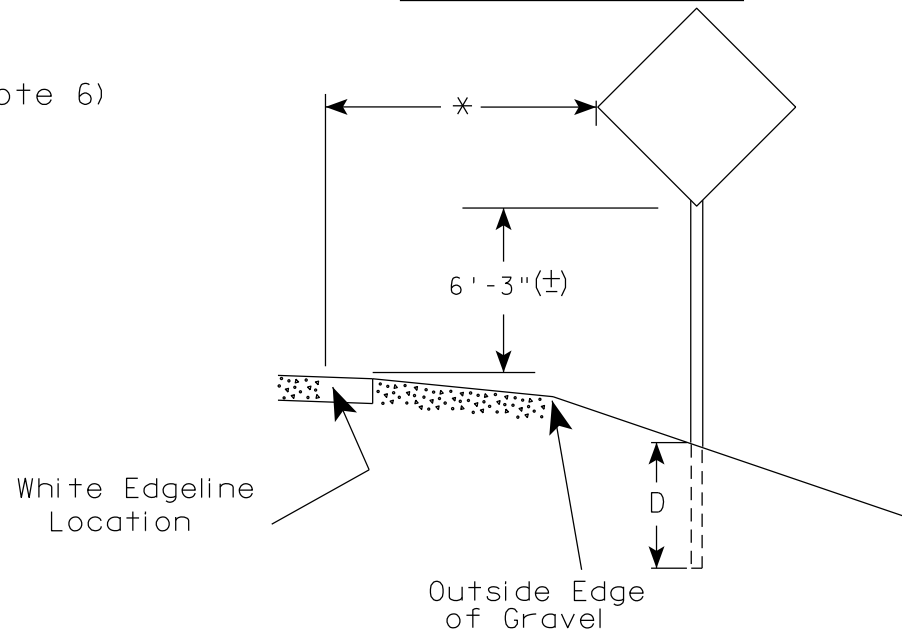
SHEET NO:

E

URBAN AREA



RURAL AREA (See Note 2)



POST EMBEDMENT DEPTH

Area of Sign Installation ( Sq. Ft. )	D ( Min )
20 or Less	4'
Greater than 20	5'

GENERAL NOTES

1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on or behind barrier wall, see A4-10 sign plate.  
The Double Arrow sign (W12-1D) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).
3. For expressways and freeways, mounting height is 7'- 3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
4. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±).
5. Offset distance shall be consistent with existing signs or consistent throughout length of project.
6. The (±) tolerance for mounting height is 3 inches.
7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the Engineer.

\* \* The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

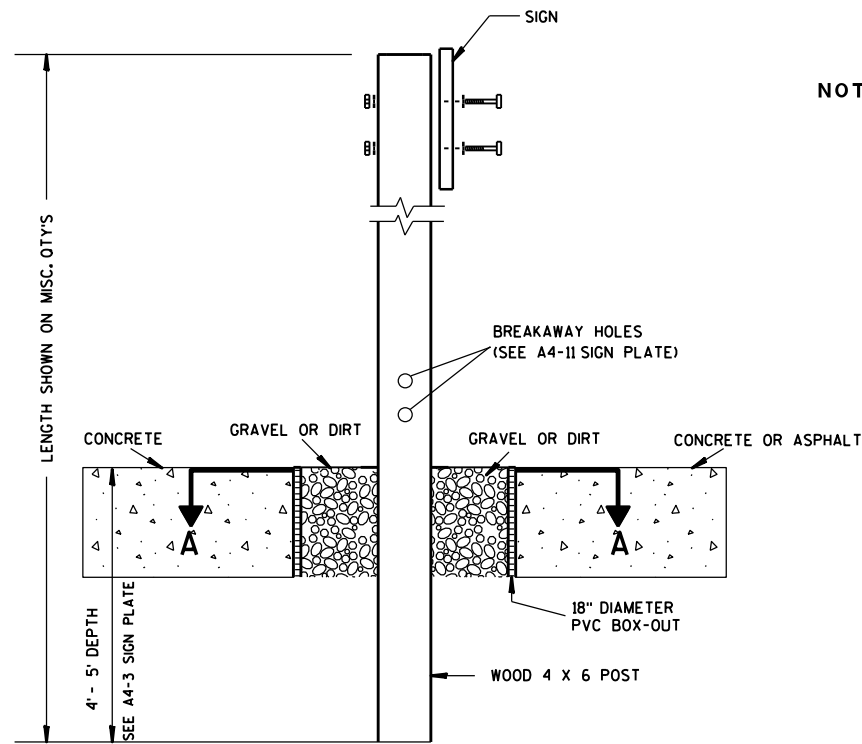
\* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

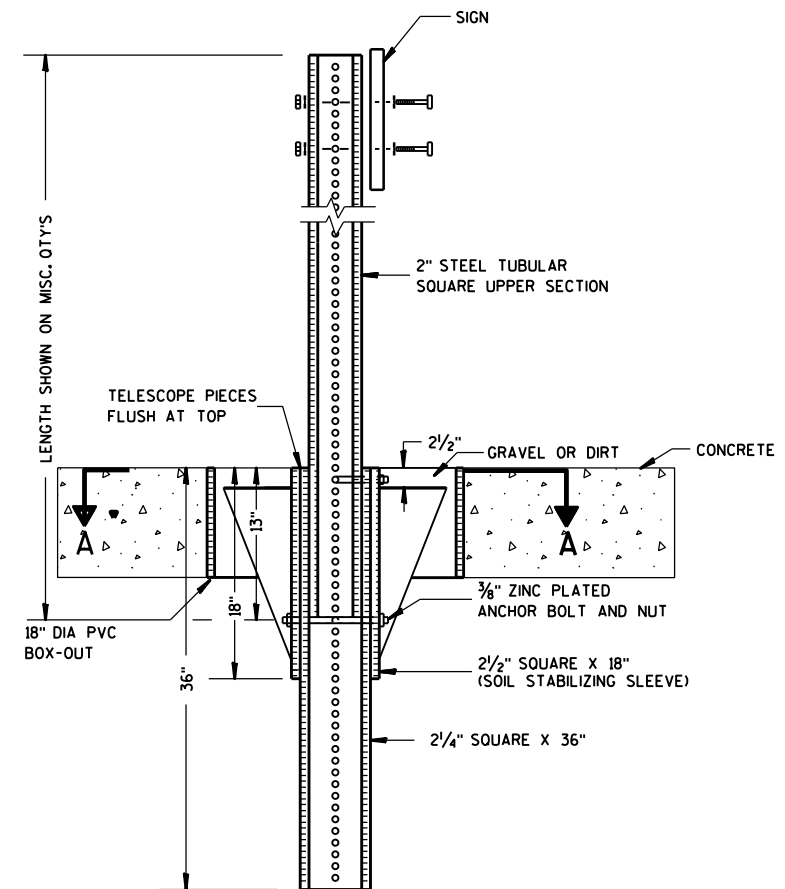
DATE 5/13/2020 PLATE NO. A4-3.22



**ELEVATION VIEW**

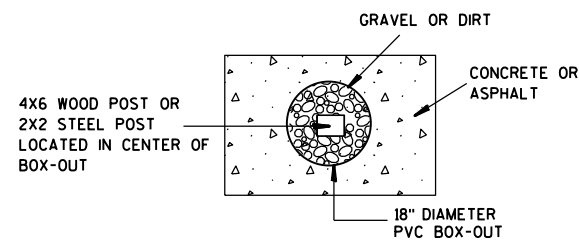
**DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT**

- NOTES:**
1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
  2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
  3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



**ELEVATION VIEW**

**DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT**



**PLAN VIEW**

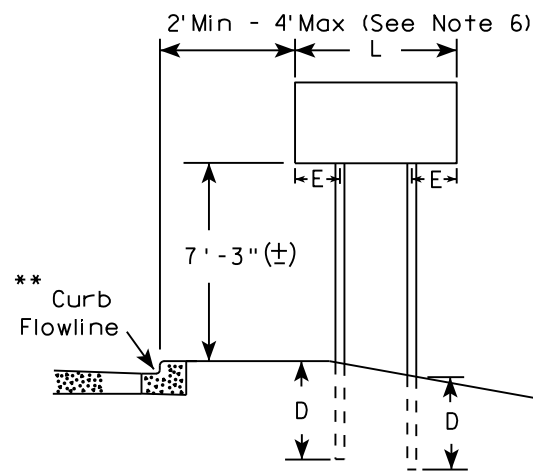
**FOR NEW CONCRETE/ ASPHALT INSTALLATIONS**

<b>SIGN POST BOX-OUTS A4-3B</b>	
<small>WISCONSIN DEPT OF TRANSPORTATION</small>	
APPROVED <i>Matthew R. Rauch</i> <small>for State Traffic Engineer</small>	
<small>DATE 1/27/14</small>	<small>PLATE NO. A4-3B.1</small>

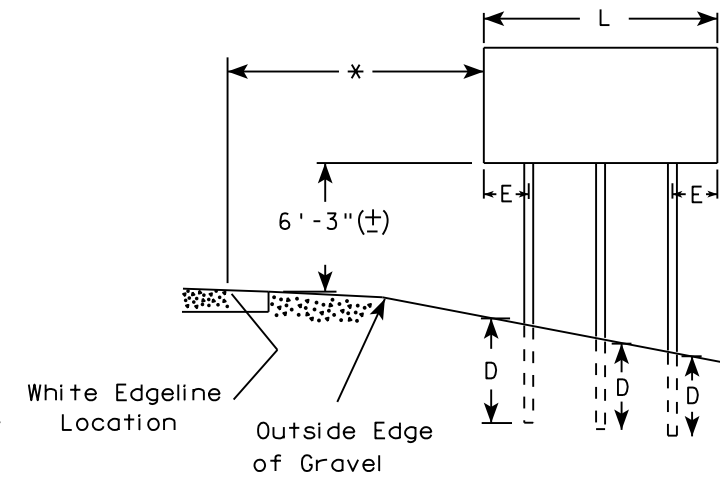
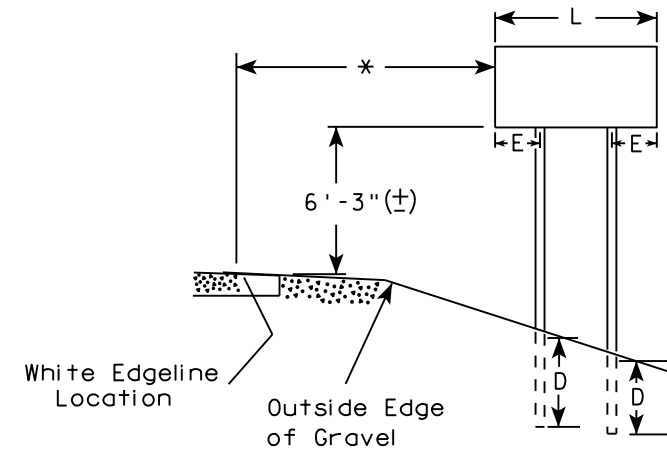
GENERAL NOTES

1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
2. See tables below for required number of posts.
3. For expressways and freeways, mounting height is 7'-3" (±) or 6'-3" (±) depending upon existence of sub-sign.
4. The (±) tolerance for mounting height is 3 inches.
5. J-Assemblies are considered to be one sign for mounting height.
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the engineer.
8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).

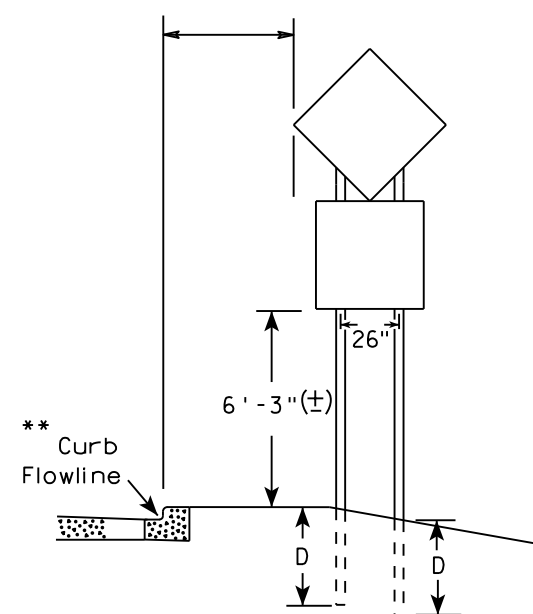
URBAN AREA



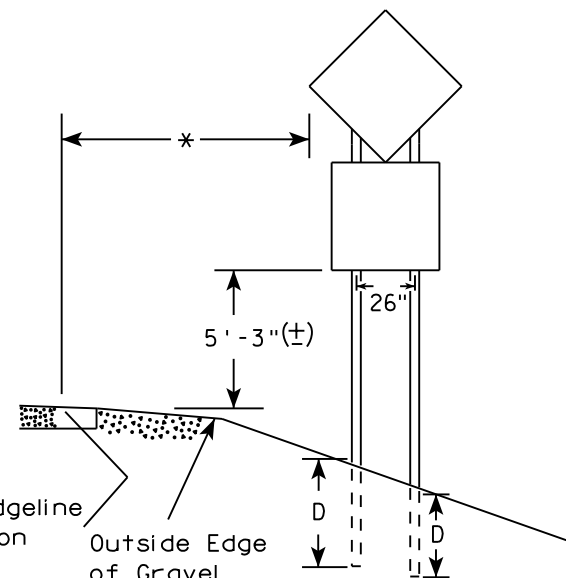
RURAL AREA (See Note 3)



2' Min - 4' Max (See Note 6)



48" DIAMOND WARNING SIGN



48" DIAMOND WARNING SIGN

\* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

\*\* The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

\*\*\* See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)	
L	E
Greater than 48" Less than 60"	12"
60" to 108"	L/5

\*\*\*

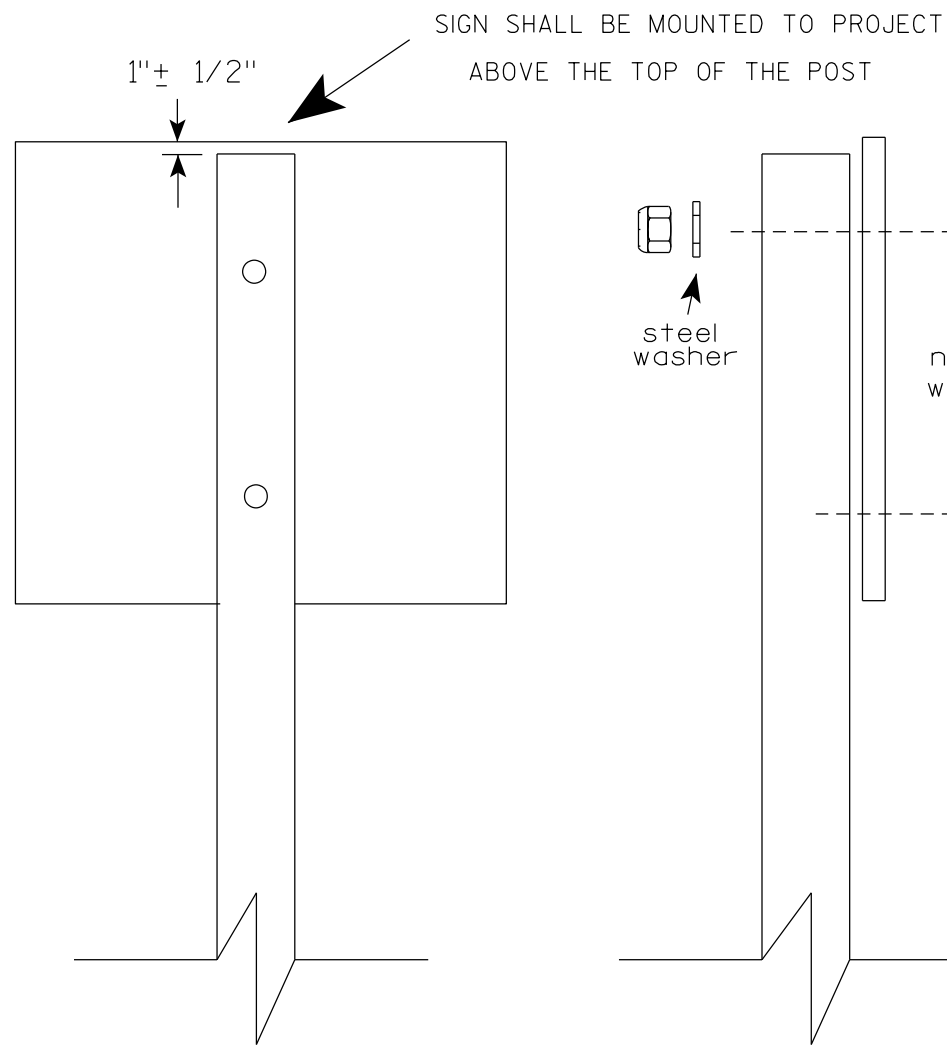
SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)	
L	E
Greater than 108" to 144"	12"

POST EMBEDMENT DEPTH

Area of Sign Installation ( Sq. Ft. )	D ( Min )
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS

WISCONSIN DEPT OF TRANSPORTATION  
 APPROVED *Matthew R. Rauch*  
 For State Traffic Engineer  
 DATE 8/21/17 PLATE NO. A4-4.15



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

- a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
- b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)

MACHINE BOLTS - 5/16" X 1-3/4" Length w/ lock nuts

WOOD POSTS (4" x 6")

LAG SCREWS - 3/8" X 3" (NO STRINGERS ON BACK OF SIGN)  
3/8" X 4" (STRINGERS ON BACK OF SIGN)

SQUARE STEEL POSTS (2" x 2")

MACHINE BOLTS - 3/8" X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)  
3/8" X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)

RIVETS - 9/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL  
O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH

WASHERS (ALL POSTS) -

1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL

1-1/4" O.D. X 3/8" I.D. X .080 NYLON

\* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS  
TO POSTS

WISCONSIN DEPT OF TRANSPORTATION

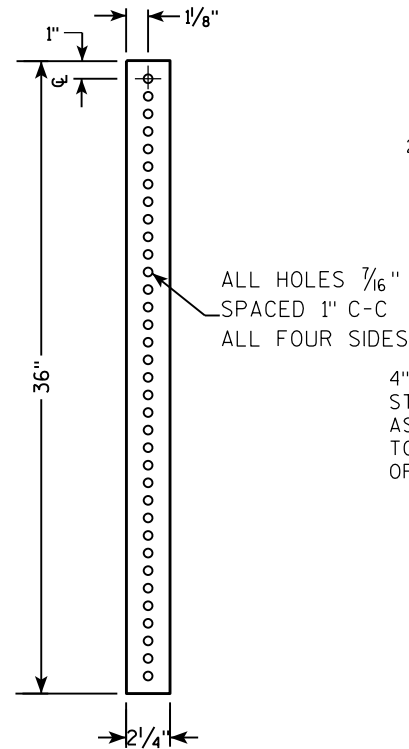
APPROVED *Matthew R Rauch*  
For State Traffic Engineer

DATE 4/1/2020 PLATE NO. A4-8.9

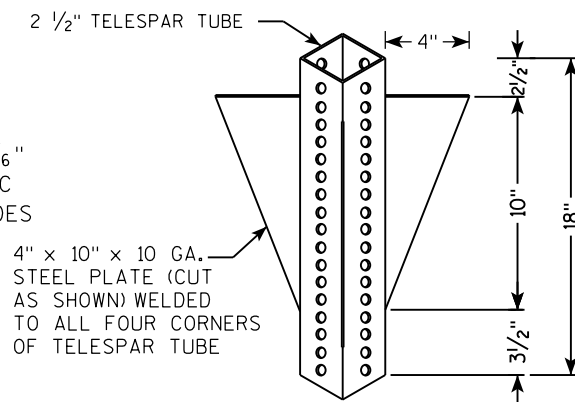


**TELESCOPIC TUBING ANCHORS  
TWO PIECE SYSTEM**

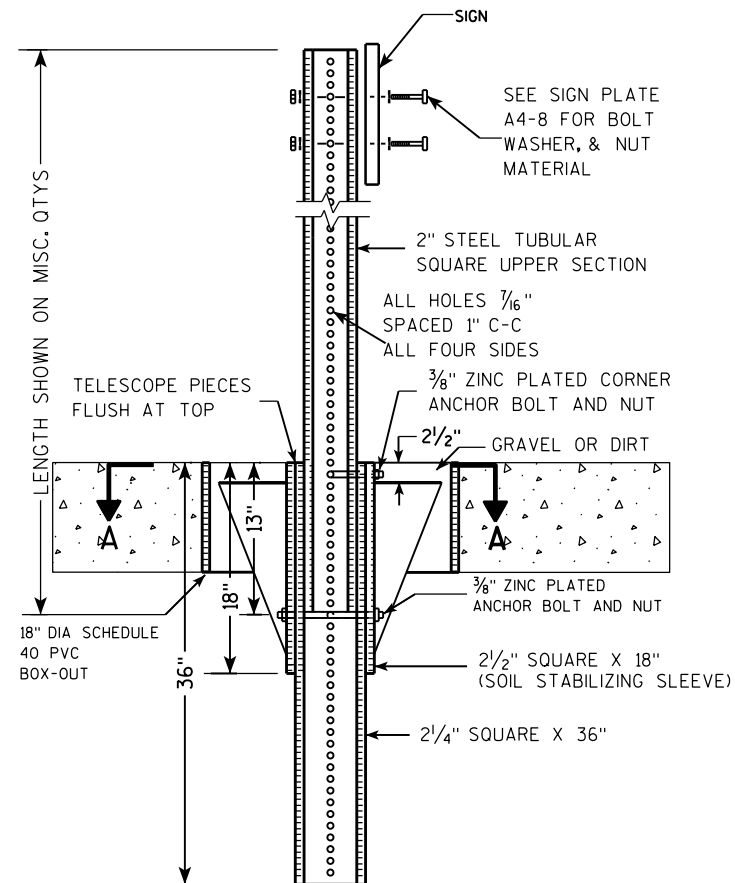
**2 1/4" SQUARE  
12 GAUGE  
PERFORATED  
GALVANIZED FINISH**



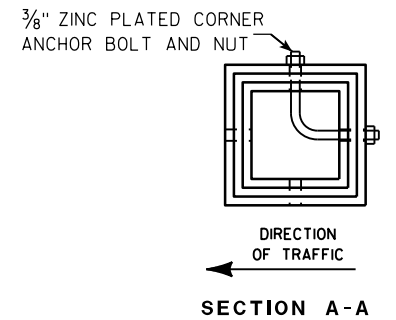
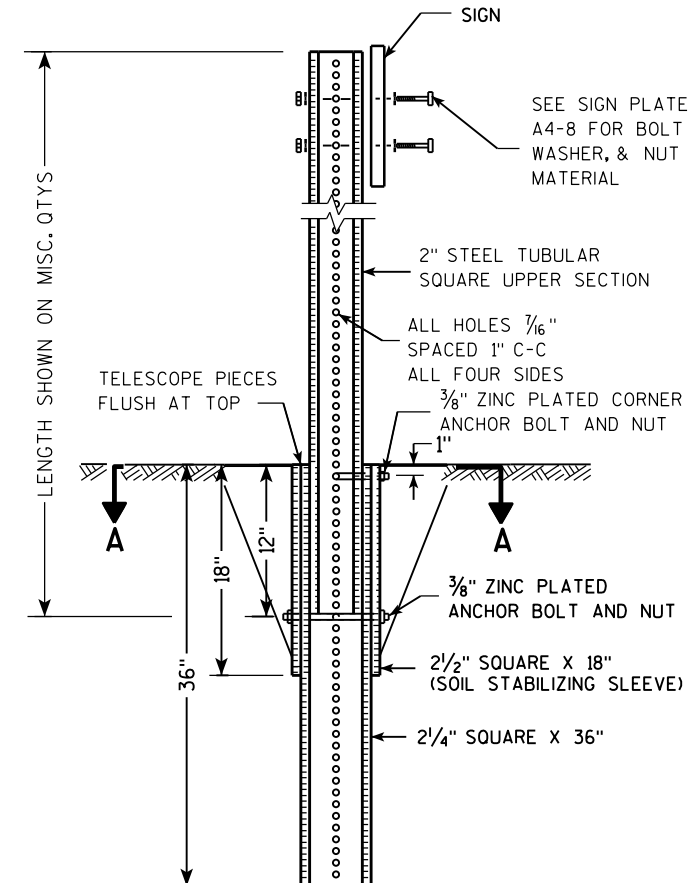
**2 1/2" SQUARE  
12 GAUGE  
OMNI-DIRECTIONAL  
PERFORATED  
SOIL STABILIZING SLEEVE  
GALVANIZED FINISH**



**DETAIL OF TUBULAR STEEL SIGN POST  
(IN POURED CONCRETE OR ASPHALT)**



**DETAIL OF TUBULAR STEEL SIGN POST  
(IN LOCATIONS OTHER THAN POURED CONCRETE OR ASPHALT)**



Area of Sign Installation (Sq. Ft.)	Number of Required Posts
9 or less	1
Greater than 9 less than or equal to 18	2
Greater than 18 less than or equal to 27	3

Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

**TUBULAR STEEL  
SIGN POST  
A4-9**

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 2/05/15 PLATE NO. A4-9.9

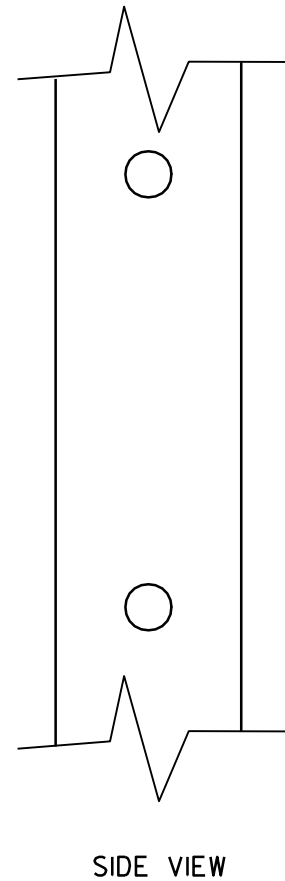
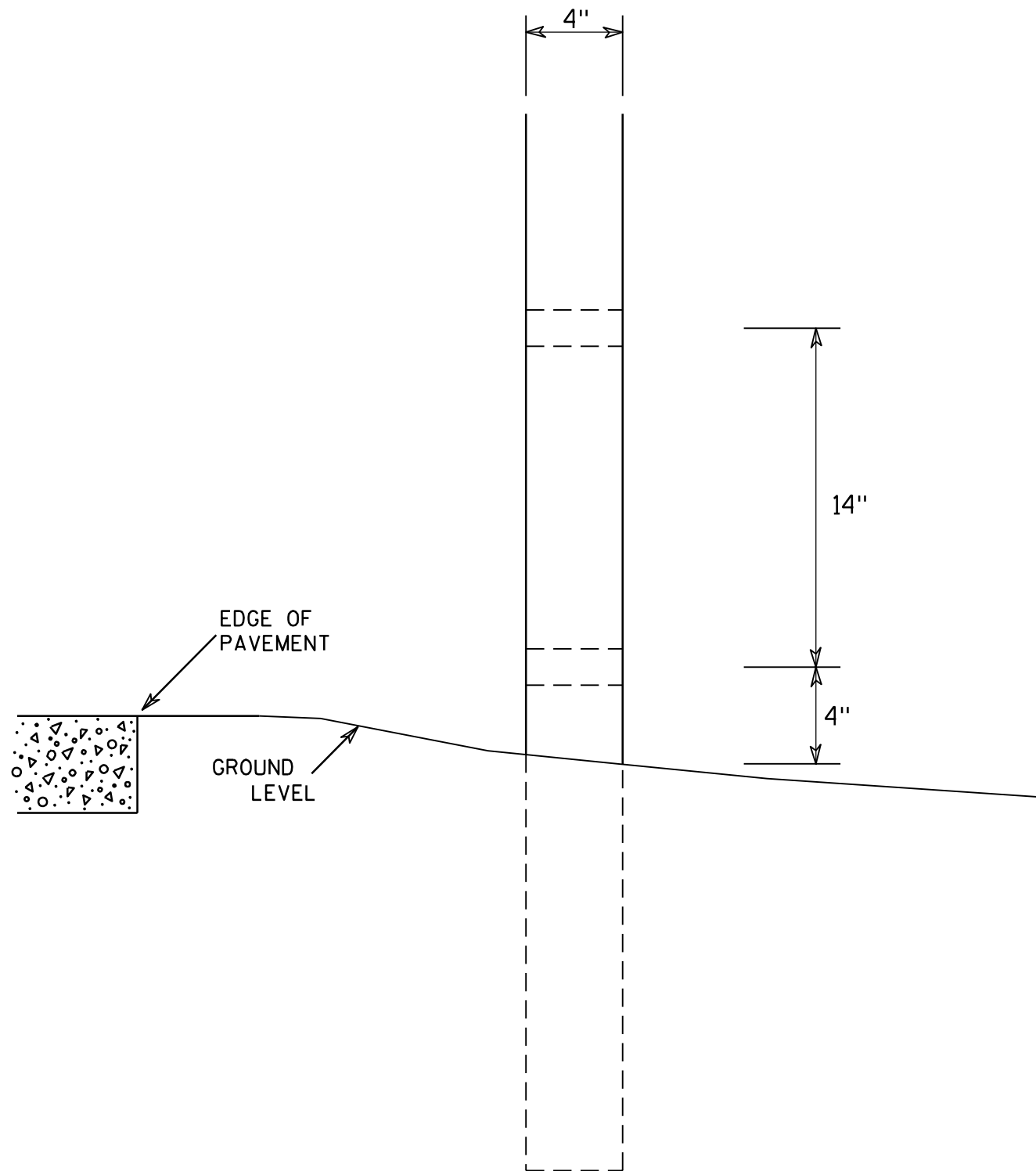
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E



GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

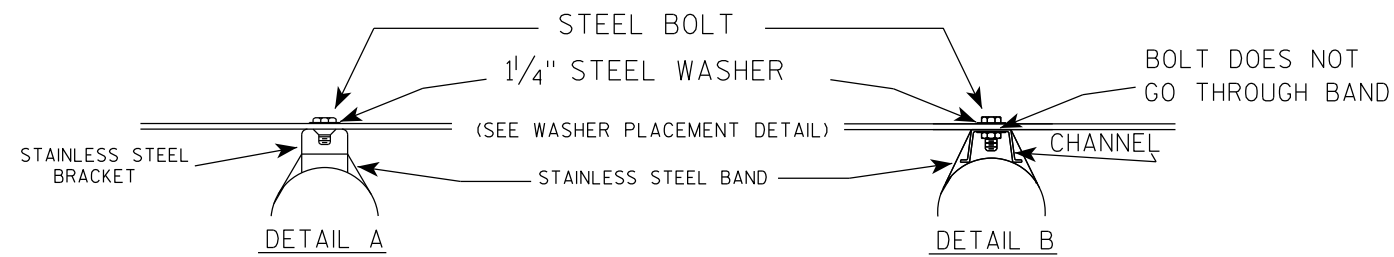
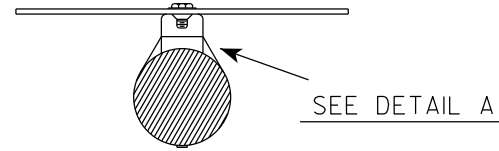
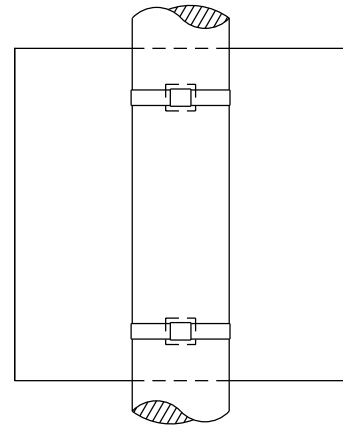
7

7

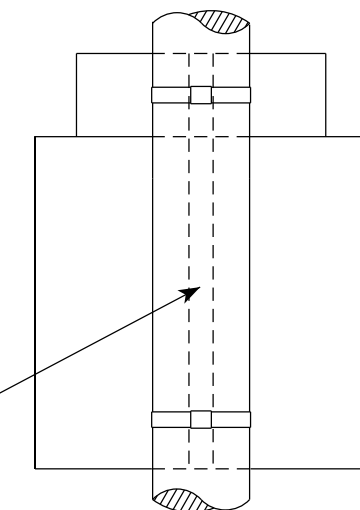
<b>4 X 6 WOOD POST MODIFICATIONS</b>	
<i>WISCONSIN DEPT OF TRANSPORTATION</i>	
APPROVED	<i>Chester J Spang</i> for State Traffic Engineer
DATE <u>3/27/97</u>	PLATE NO. <u>A4-11.2</u>

# BANDING

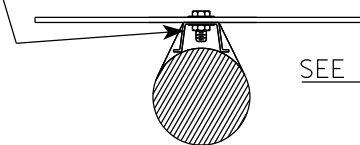
SINGLE SIGN



"J" ASSEMBLY

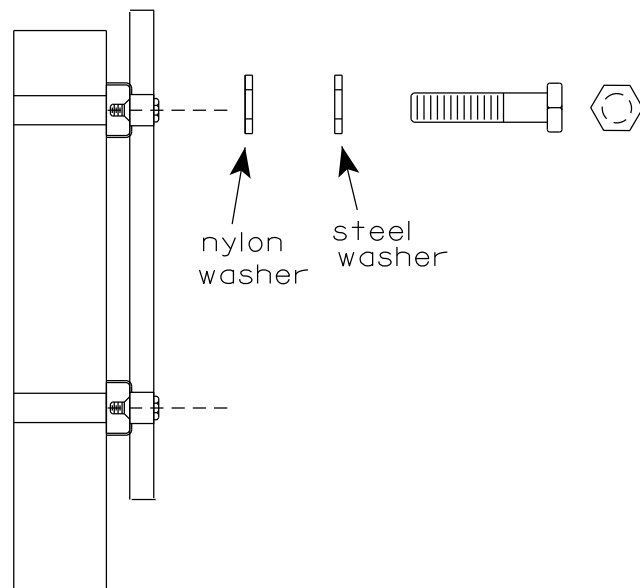


CHANNEL  
SEE TYPICAL PANEL  
INSTALLATION SHEET



- GENERAL NOTES**
1. Any sign over 3 feet in width shall use the V-Block banding method. See A5-10 standard plate.
  2. Signs 3 feet or greater in height shall have three bracket bands installed. Signs less than 3 feet in height shall have two bracket bands installed.
  3. Banding and assembly bracket shall be stainless steel. All bands shall be  $\frac{3}{4}$ " in width and 0.025" thickness.
  4. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
    - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
    - b. Electro-galvanized in accordance with ASTM designation: B 633, Type III, SC 3

WASHER PLACEMENT



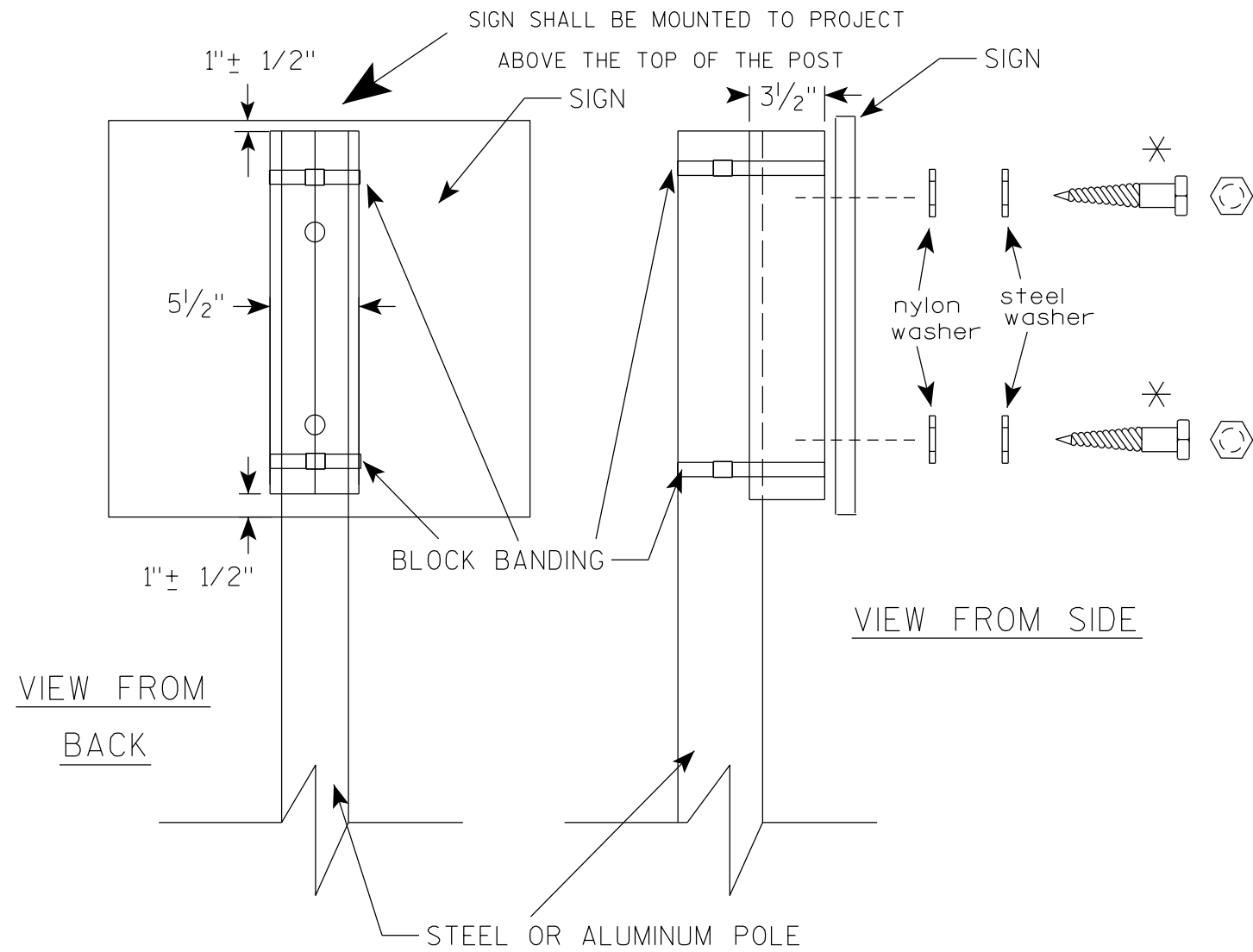
WASHERS (ALL POSTS) -  
 1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL  
 1-1/4" O.D. X 3/8" I.D. X .080 NYLON  
 FOR ALL TYPE H SIGNS

STANDARD SIGN  
SIGN BANDING DETAILS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

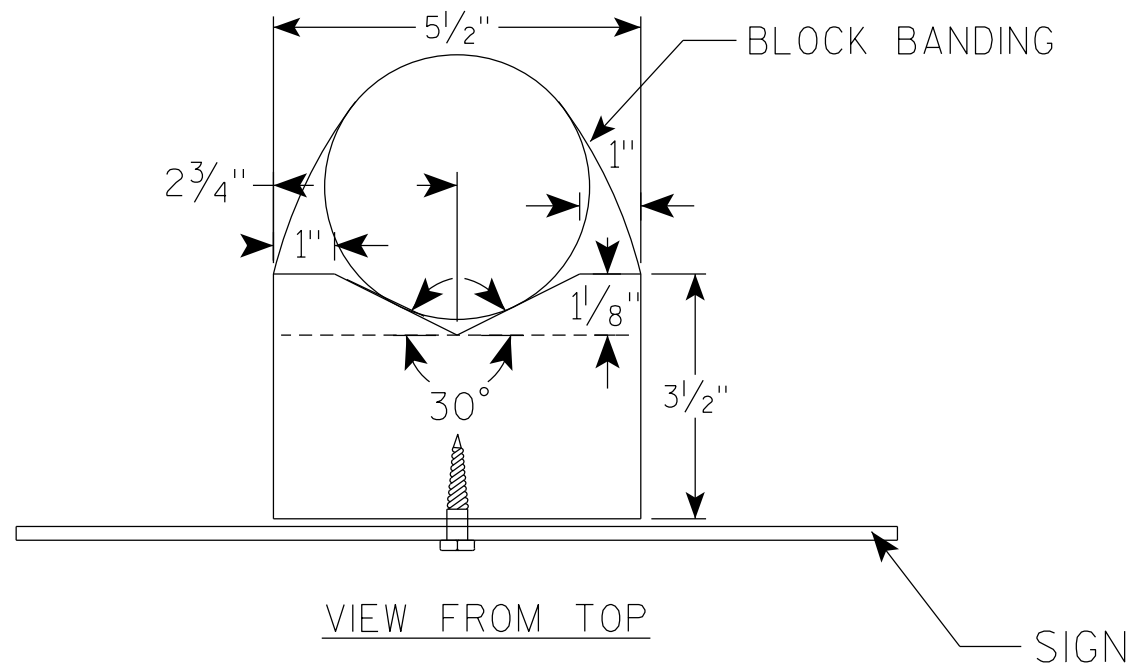
DATE 6/10/19 PLATE NO. A5-9.4



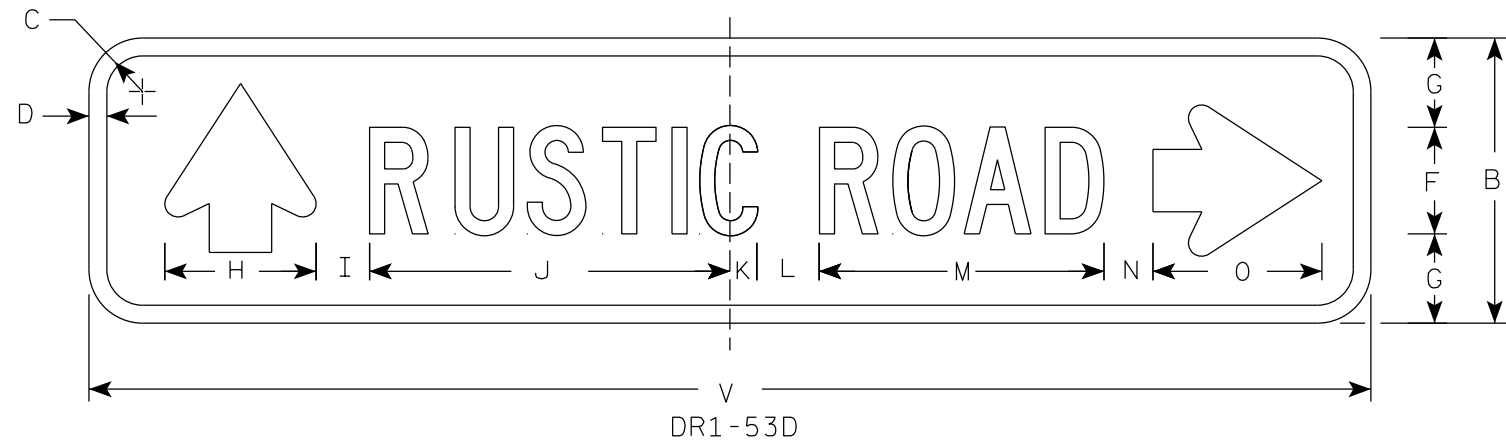
GENERAL NOTES

1. WOOD 4"X6" POST MATERIAL SHALL CONFORM TO 507.2.2 OF THE WISDOT STANDARD SPECIFICATIONS
2. BLOCK BANDING AND CLIPS SHALL BE STAINLESS STEEL, 3/4" WIDTH AND 0.025" THICKNESS
3. SIGNS 3' OR GREATER IN HEIGHT SHALL UTILIZE 3 BLOCK BANDS. SIGNS UNDER 3' IN HEIGHT SHALL UTILIZE 2 BLOCK BANDS
4. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA, BUT NORMALLY THERE ARE TWO. FOR SIGNS GREATER THAN 9 S.F. 3 FASTENERS SHALL BE USED.
5. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
  - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
  - b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3
6. ALL BOLTS SHALL HAVE HEXAGONAL HEADS.
7. STEEL WASHERS SHALL BE 1/4" O.D. X 3/8" I.D. X 1/16"
8. NYLON WASHERS SHALL BE 1/4" O.D. X 3/8" I.D. X .080 FOR TYPE H OR TYPE F FACE SIGN

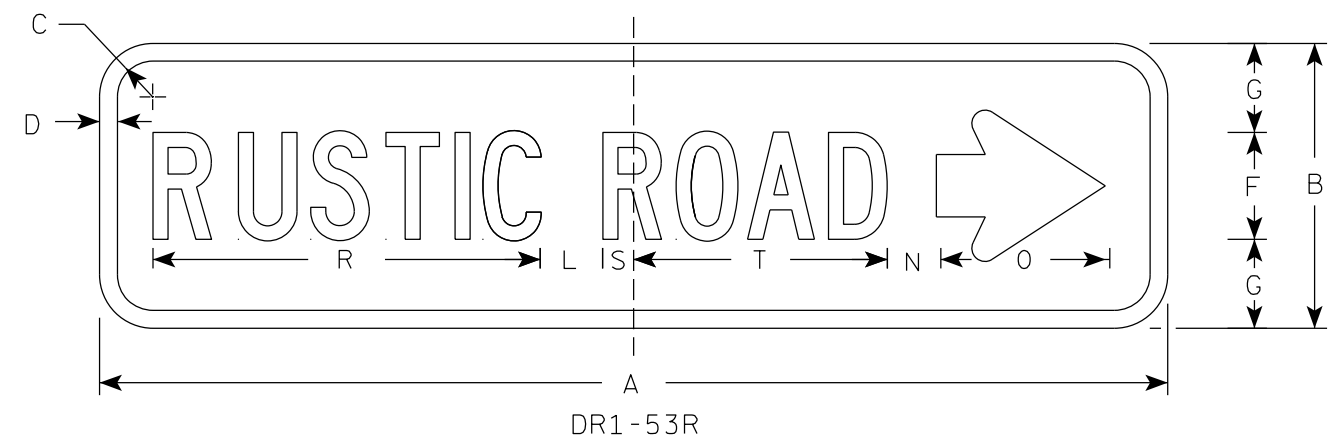
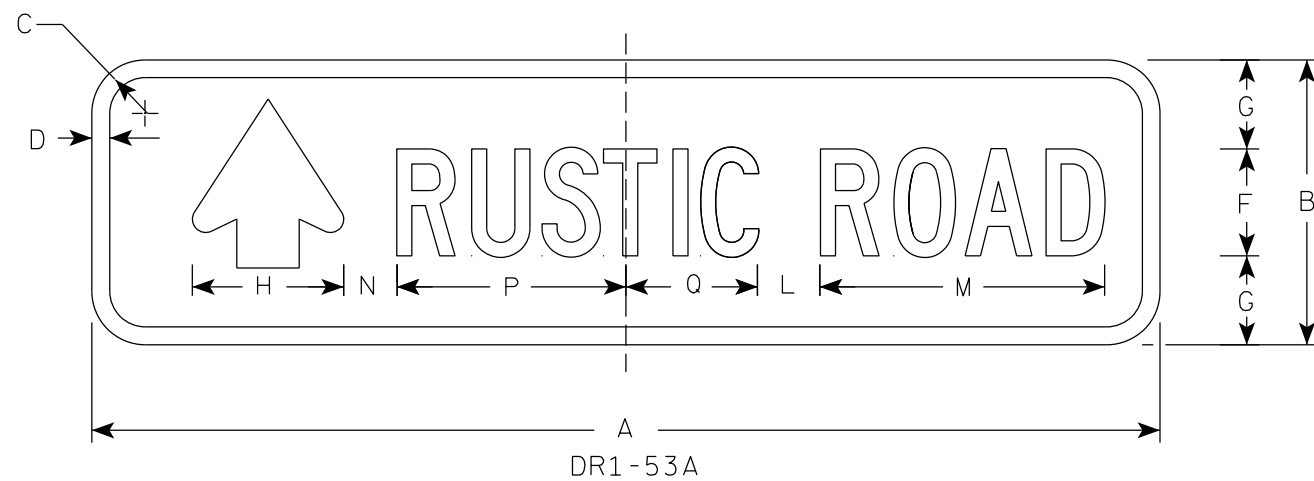
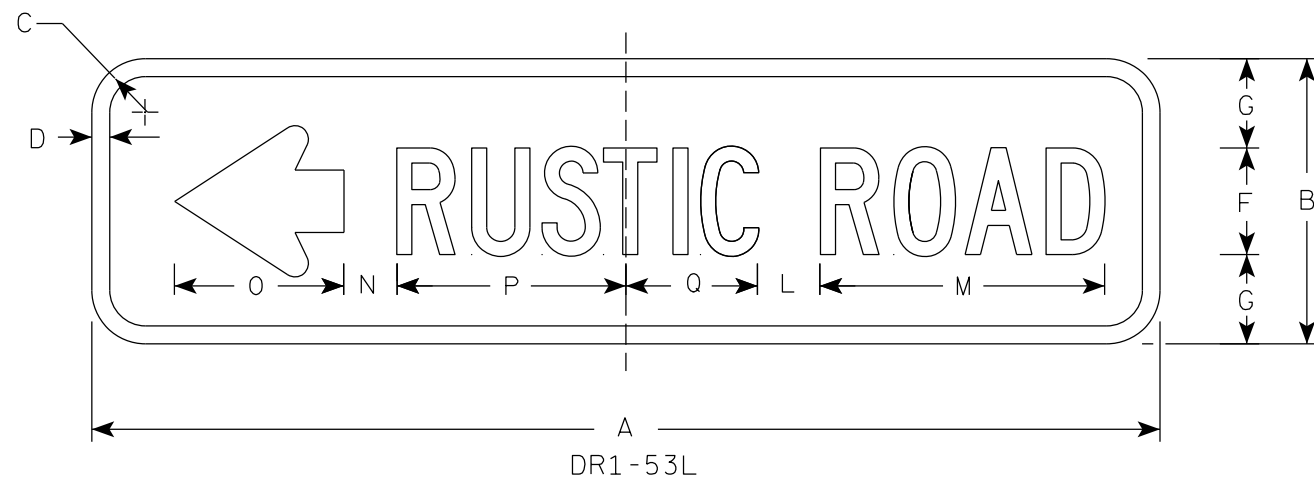
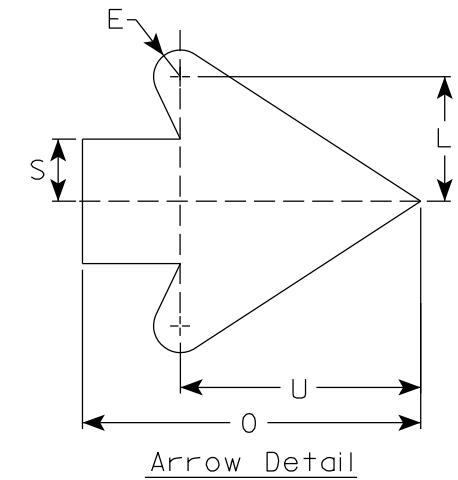
✱ LAG BOLTS SHALL BE 3/8" X 2 1/2"



BLOCK BANDING DETAIL ( V-BLOCK OPTION )	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R Rauch</i> for State Traffic Engineer
DATE 4/19/2022	PLATE NO. A5-10.3



- NOTES
1. Sign is Type II - Type H Reflective
  2. Color:  
Background - Brown  
Message - Yellow
  3. Message Series - C



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	DR1-53A	DR1-53D	
																											DR1-53L	DR1-53R	Area sq. ft.
1																													
2	30	8	1 1/8	1/2		3	2 1/2	4 1/4	1 1/2	10 1/8	3/4	1 3/4	8	1 3/8	4 3/4	6 3/8	3 3/4	10 7/8	7/8	7 1/8	3 3/8	36					1.67	2.00	
3																													
4																													
5																													

STANDARD SIGN  
DR1-53

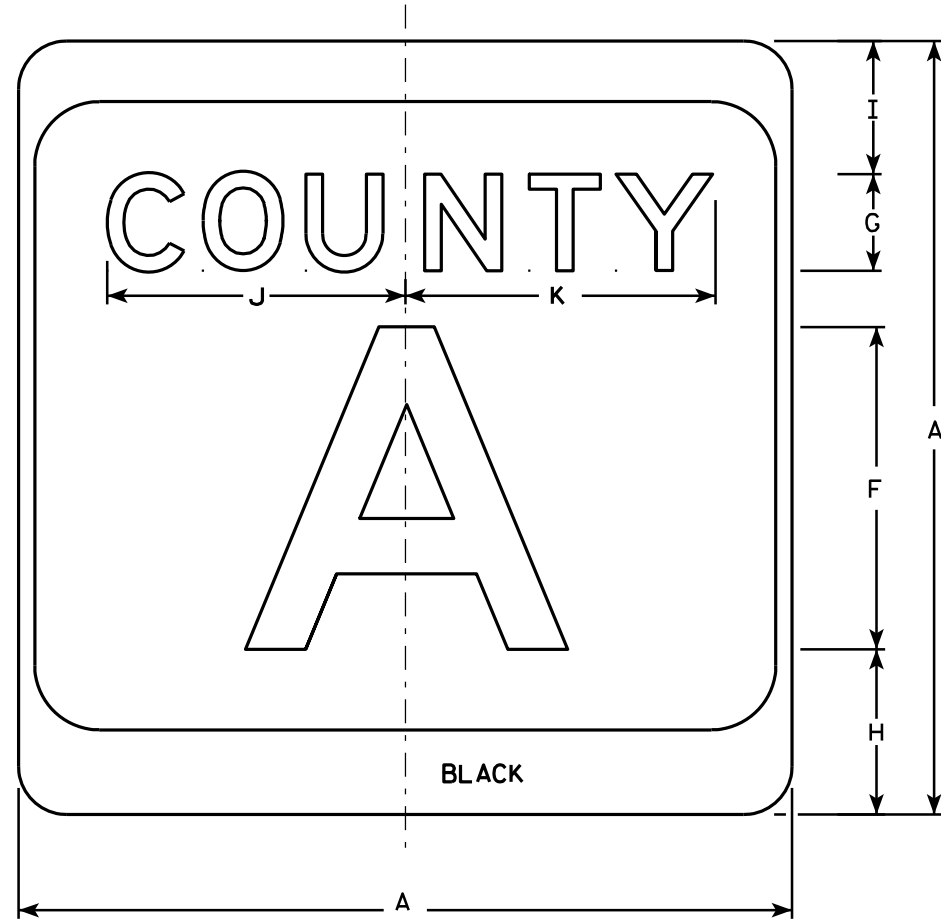
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Rauch*  
For State Traffic Engineer

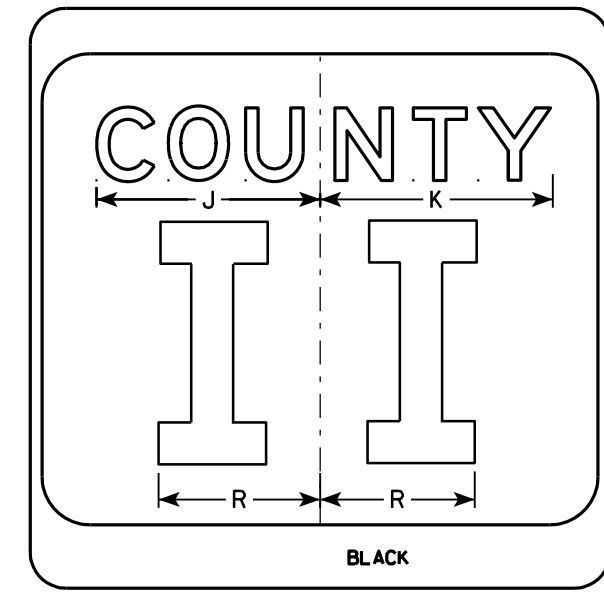
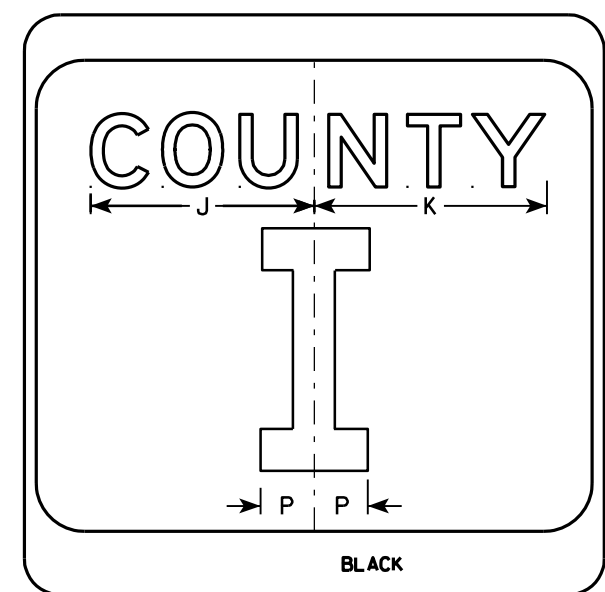
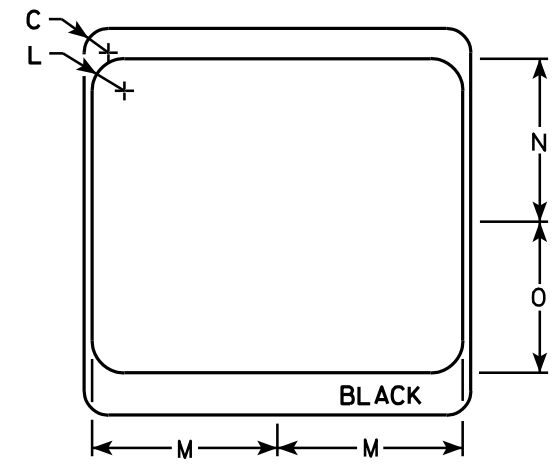
DATE 4/18/19 PLATE NO. DR1-53.6

**NOTES**

1. Sign is Type II - see Note 7 - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - White & Black - See Note 7  
Message - Black
3. Message Series - see Note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Message Series E for 1 letter.  
Message Series D for 2 letters unless message is too big then Series C.  
Message Series C for 3 letters unless message is too big then Series B.
6. Substitute appropriate letters & optically center to achieve proper balance.
7. Permanent Signs  
Background - Type H Reflective  
Detour or temporary Signs  
Background - Reflective



M1-5A



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24		1 1/2			10	3	5 1/8	4 1/8	9 1/4	9 5/8	2	11 1/2	10 1/8	9 3/8	2 1/4		6 5/8									4.0
3	36		2 1/4			16	4	7 5/8	5 5/8	12 1/4	12 7/8	3	17 1/8	15 1/4	14	3 3/8		10									9.0
4	36		2 1/4			16	4	7 5/8	5 5/8	12 1/4	12 7/8	3	17 1/8	15 1/4	14	3 3/8		10									9.0
5	36		2 1/4			16	4	7 5/8	5 5/8	12 1/4	12 7/8	3	17 1/8	15 1/4	14	3 3/8		10									9.0

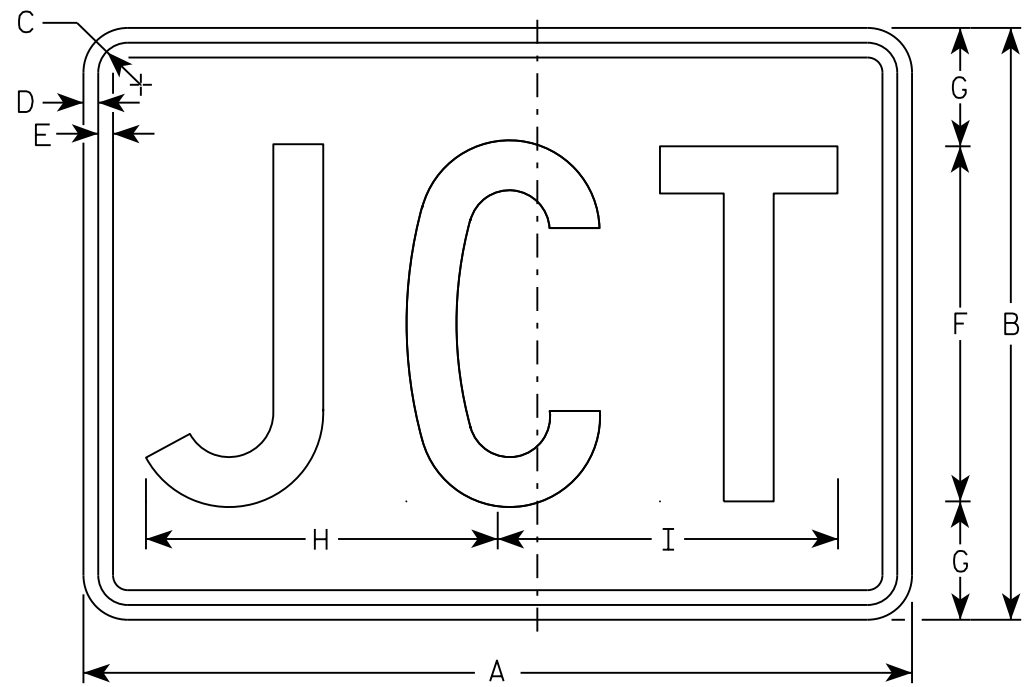
**CTH MARKER**  
**M1-5A FOR ASSEMBLIES**

WISCONSIN DEPT OF TRANSPORTATION

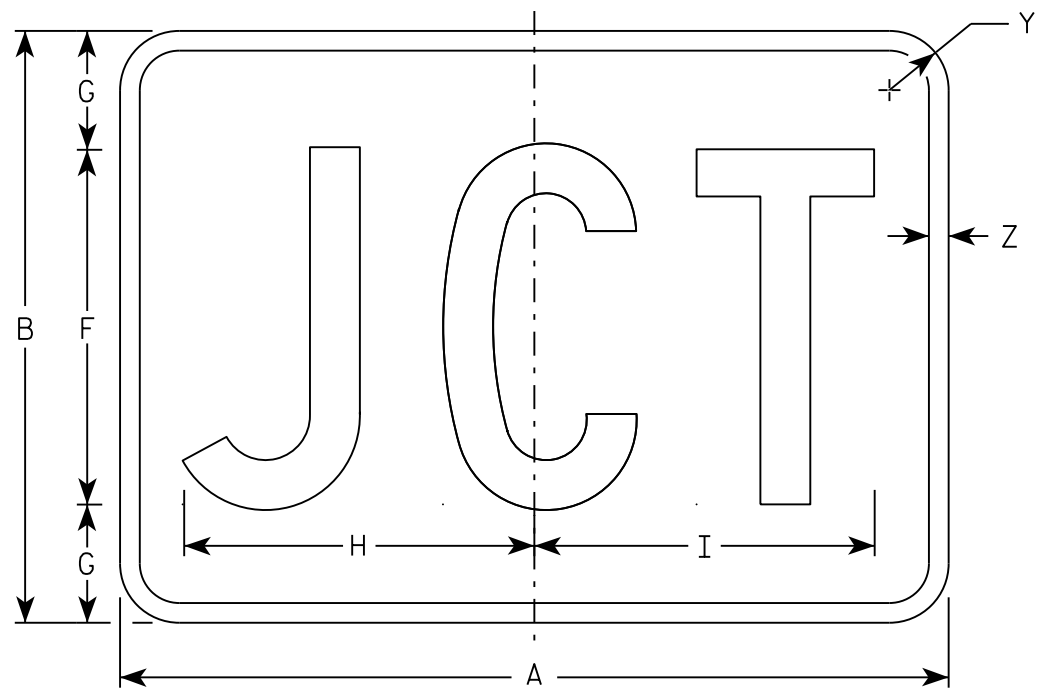
APPROVED *Matthew R. Raub*  
For State Traffic Engineer

DATE 9/27/11 PLATE NO. MI-5A.8

PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: **E**



M2-1  
MM2-1  
MP2-1



MB2-1  
MK2-1  
MN2-1  
MR2-1

NOTES

1. Sign is Type II - Type H
2. Color:  
Background - See note 5  
Message - See note 5
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. M2-1 Background - White  
Message - Black  
MB2-1 Background - Blue  
Message - White  
MK2-1 Background - Green  
Message - White  
MM2-1 Background - White  
Message - Green  
MN2-1 Background - Brown  
Message - White  
MP2-1 Background - White  
Message - Blue  
MR2-1 Background - Brown  
Message - Yellow

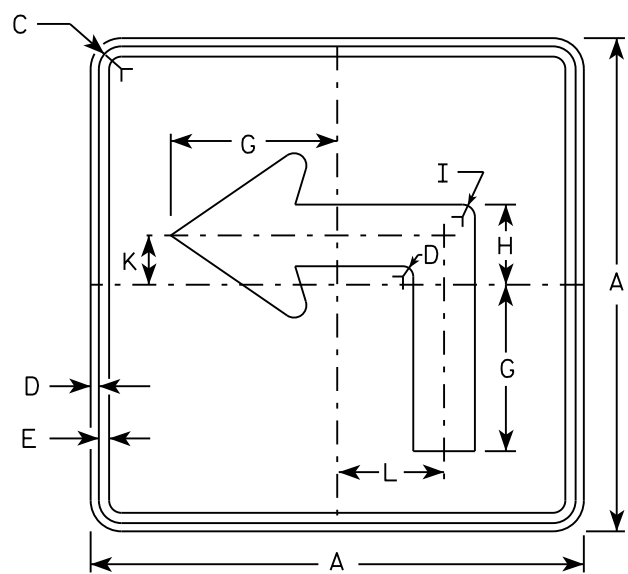
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21	15	1 1/8	3/8	3/8	9	3	8 7/8	8 5/8																1 1/2	1/2	2.20
3	30	21	1 1/8	3/8	3/8	13	4	12 7/8	12 3/8																1 1/2	1/2	4.40
4	30	21	1 1/8	3/8	3/8	13	4	12 7/8	12 3/8																1 1/2	1/2	4.40
5	30	21	1 1/8	3/8	3/8	13	4	12 7/8	12 3/8																1 1/2	1/2	4.40

STANDARD SIGN  
M2-1

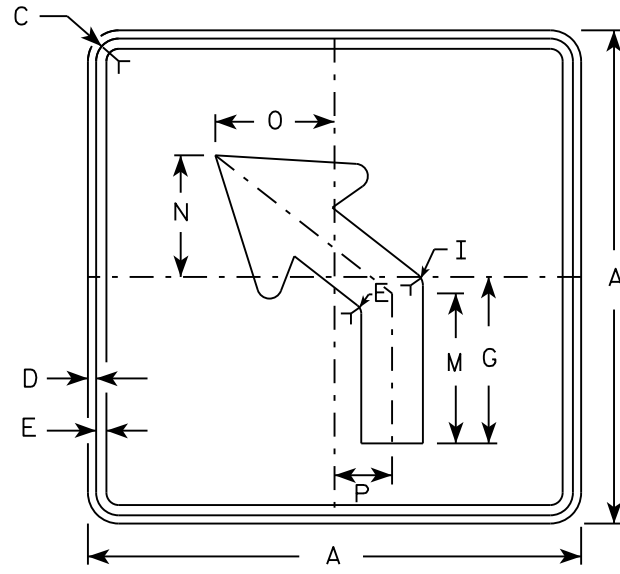
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
For State Traffic Engineer

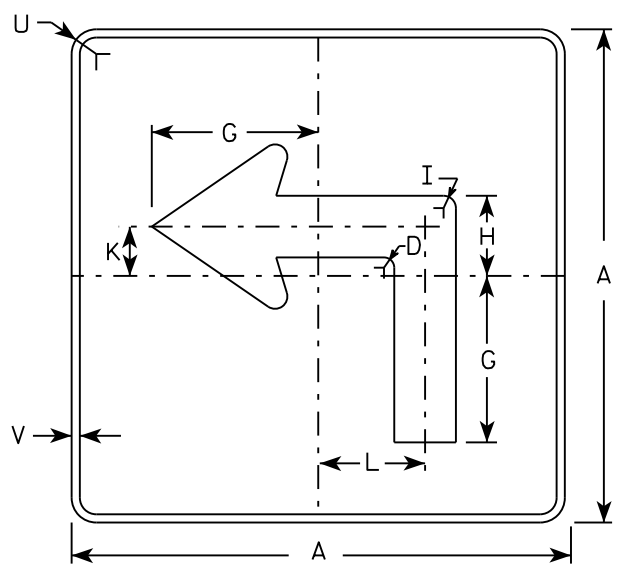
DATE 10/15/15 PLATE NO. M2-1.12



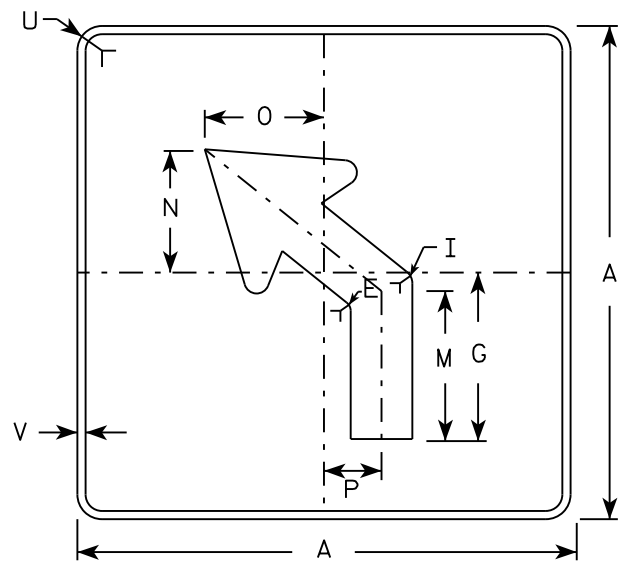
M5-1L  
MM5-1L  
M05-1L  
MP5-1L



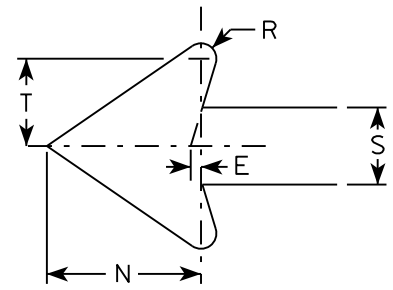
M5-2L  
MM5-2L  
M05-2L  
MP5-2L



MB5-1L  
MK5-1L  
MN5-1L  
MR5-1L



MB5-2L  
MK5-2L  
MN5-2L  
MR5-2L



NOTES

- Signs are Type II - Type H reflective except as shown
- Color:
  - Background - See note 4
  - Message - See note 4
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- M5-1 and M5-2 Background - White  
Message - Black
  - MB5-1 and MB5-2 Background - Blue  
Message - White
  - MK5-1 and MK5-2 Background - Green  
Message - White
  - MM5-1 and MM5-2 Background - White  
Message - Green
  - MN5-1 and MN5-2 Background - Brown  
Message - White
  - M05-1 and M05-2 Background - Orange - Type F Reflective  
Message - Black
  - MP5-1 and MP5-2 Background - White - Type H Reflective  
Message - Blue
  - MR5-1 and MR5-2 Background - Brown  
Message - Yellow
- M5-1R same as M5-1L except arrow points right.
- M5-2R same as M5-2L except arrow tilts right.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7	3 3/8	5/8		2 1/8	4 1/2	6 3/8	5 1/4	5	2 1/2		1/2	2 5/8	3	1 1/2	1/2					3.06
3	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25
4	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25
5	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25

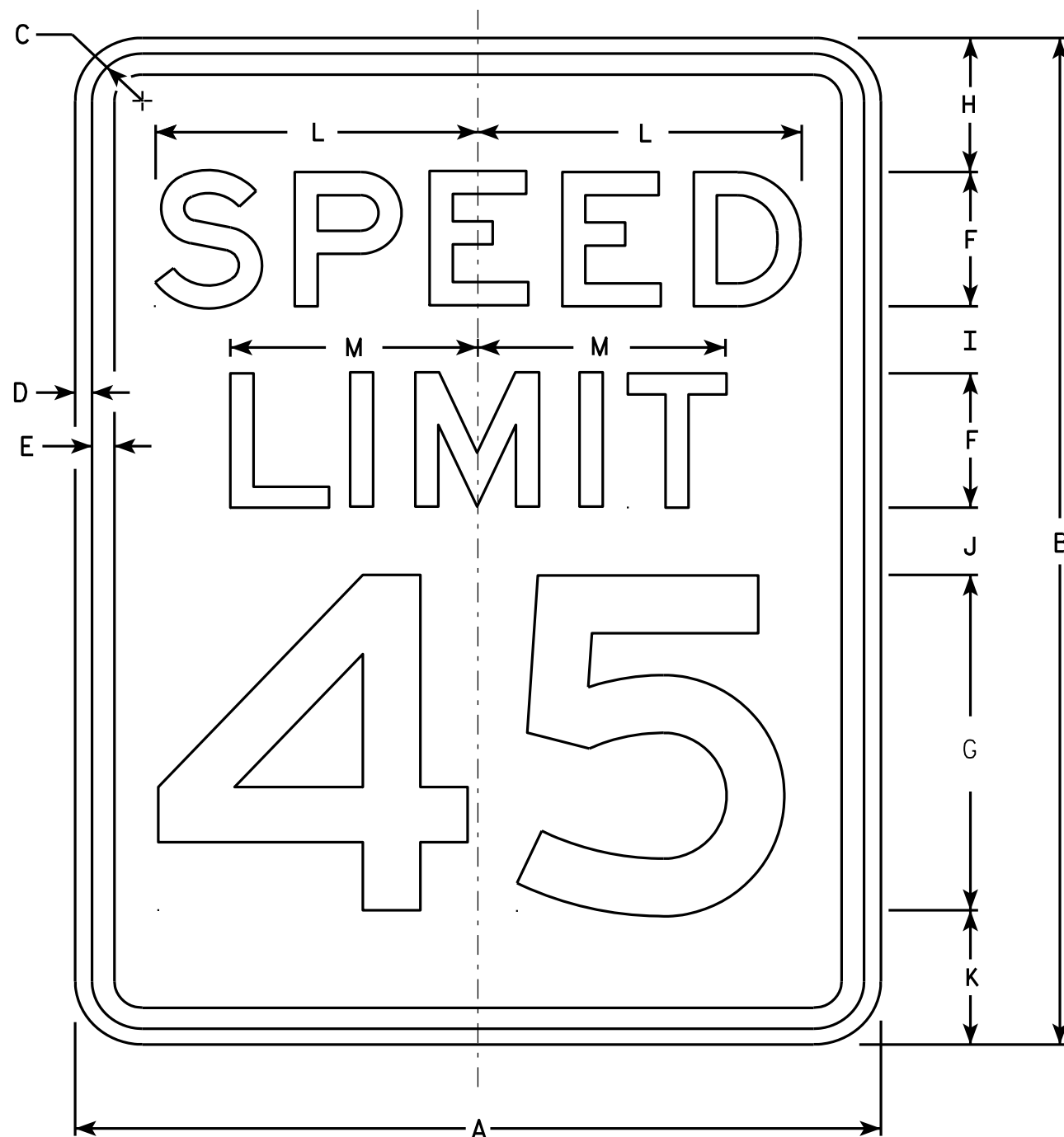
STANDARD SIGN  
M5-1 & M5-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 10/15/15 PLATE NO. M5-1.13





R2-1

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - White  
Message - Black
3. Message Series - E
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals and optically adjust spacing to achieve proper balance.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18	24	1 1/8	3/8	1/2	3	8	3	2	2	3	7 1/4	5 1/2														3.0
2S	24	30	1 1/8	3/8	1/2	4	10	3	2 1/4	3 3/8	3 3/8	9 5/8	7 3/8														5.0
2M	30	36	1 3/8	1/2	5/8	5	12	5	2 1/2	2 1/2	4	12	9 1/4														7.5
3	36	48	1 3/8	1/2	5/8	6	14	6	5	5	6	14 3/8	11														12.0
4	36	48	1 3/8	1/2	5/8	6	14	6	5	5	6	14 3/8	11														12.0
5	48	60	2 1/4	3/4	1	8	20	6	4 1/2	6 3/4	6 3/4	19 1/4	14 5/8														20.0

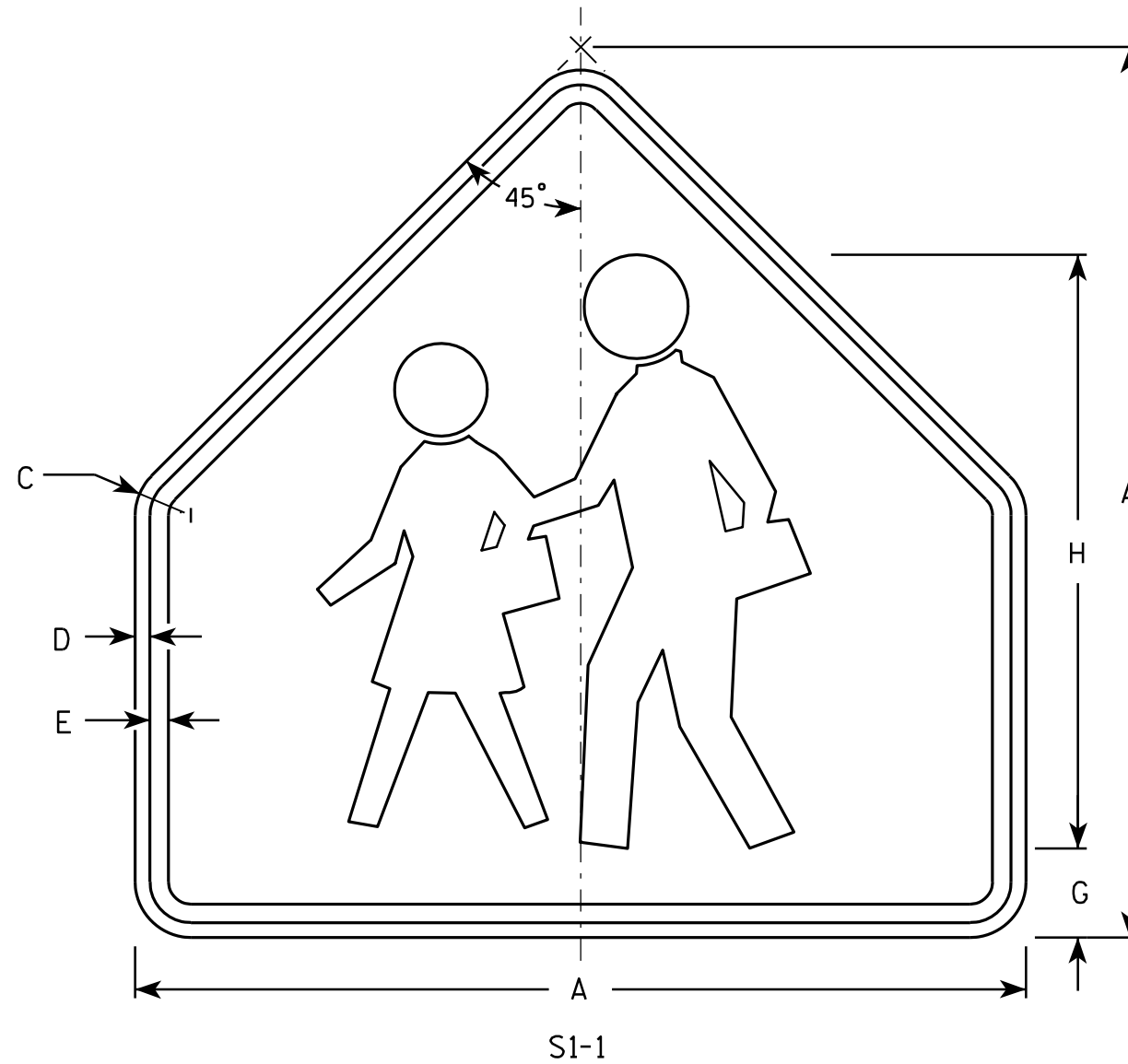
STANDARD SIGN  
R2-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
For State Traffic Engineer

DATE 5/26/10 PLATE NO. R2-1.13

PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: \_\_\_\_\_ E



**NOTES**

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - Yellow-Green  
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30		1 3/8	1/2	5/8		3	20																			4.69
2	36		1 5/8	5/8	3/4		3 1/2	24																			6.75
3	36		1 5/8	5/8	3/4		3 1/2	24																			6.75
4	48		2 1/4	3/4	1		4 3/4	32																			12
5																											

**STANDARD SIGN**  
S1-1

WISCONSIN DEPT OF TRANSPORTATION

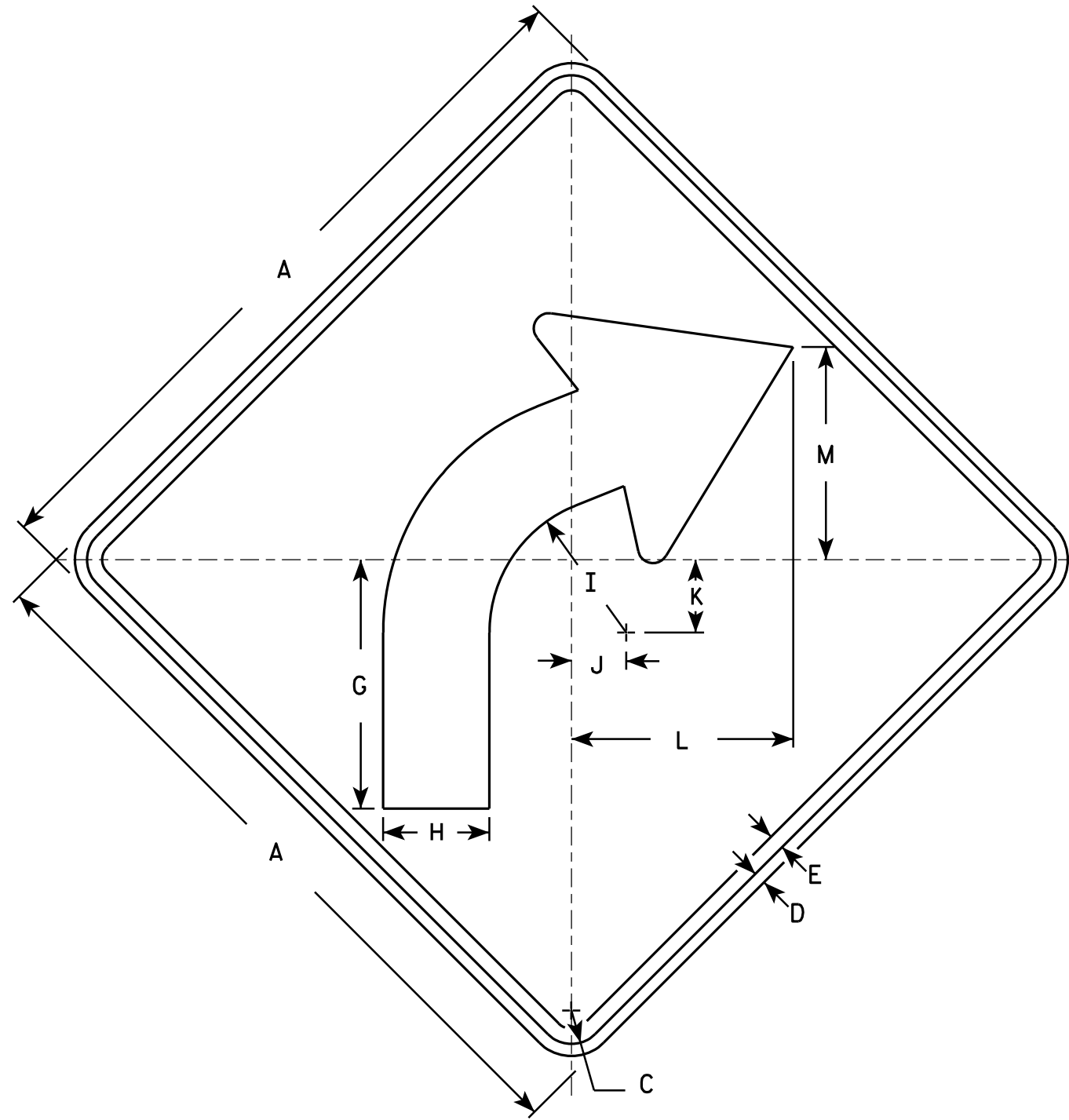
APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 6/30/05 PLATE NO. S1-1.8

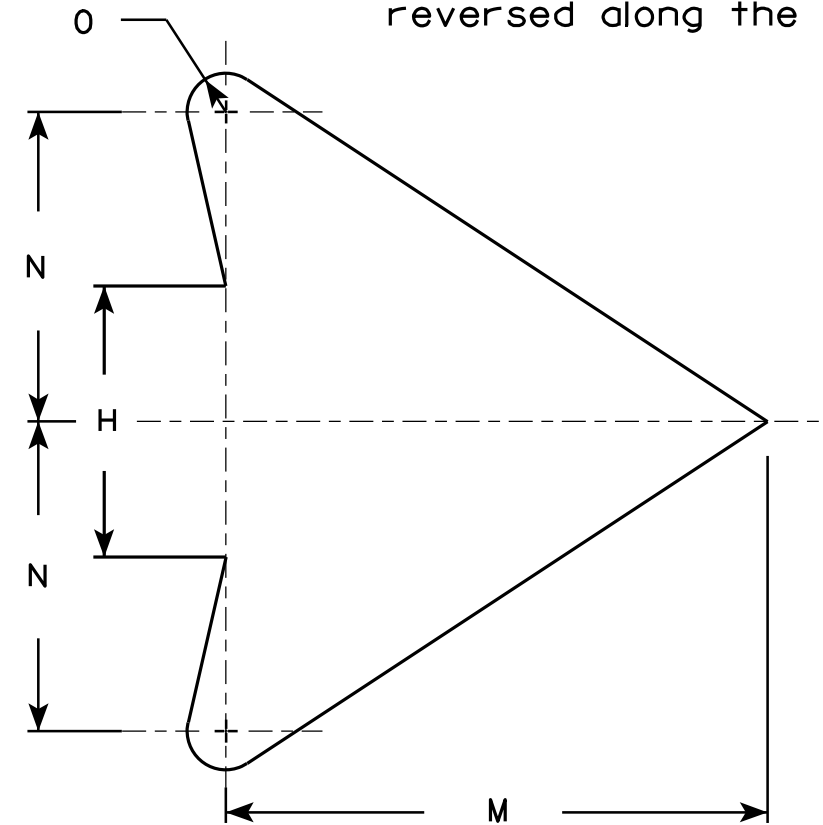
PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: \_\_\_\_\_ E

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - Yellow  
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. W1-2L is the same as W1-2R except the arrow is reversed along the vertical centerline.



W1-2R



ARROW DETAIL

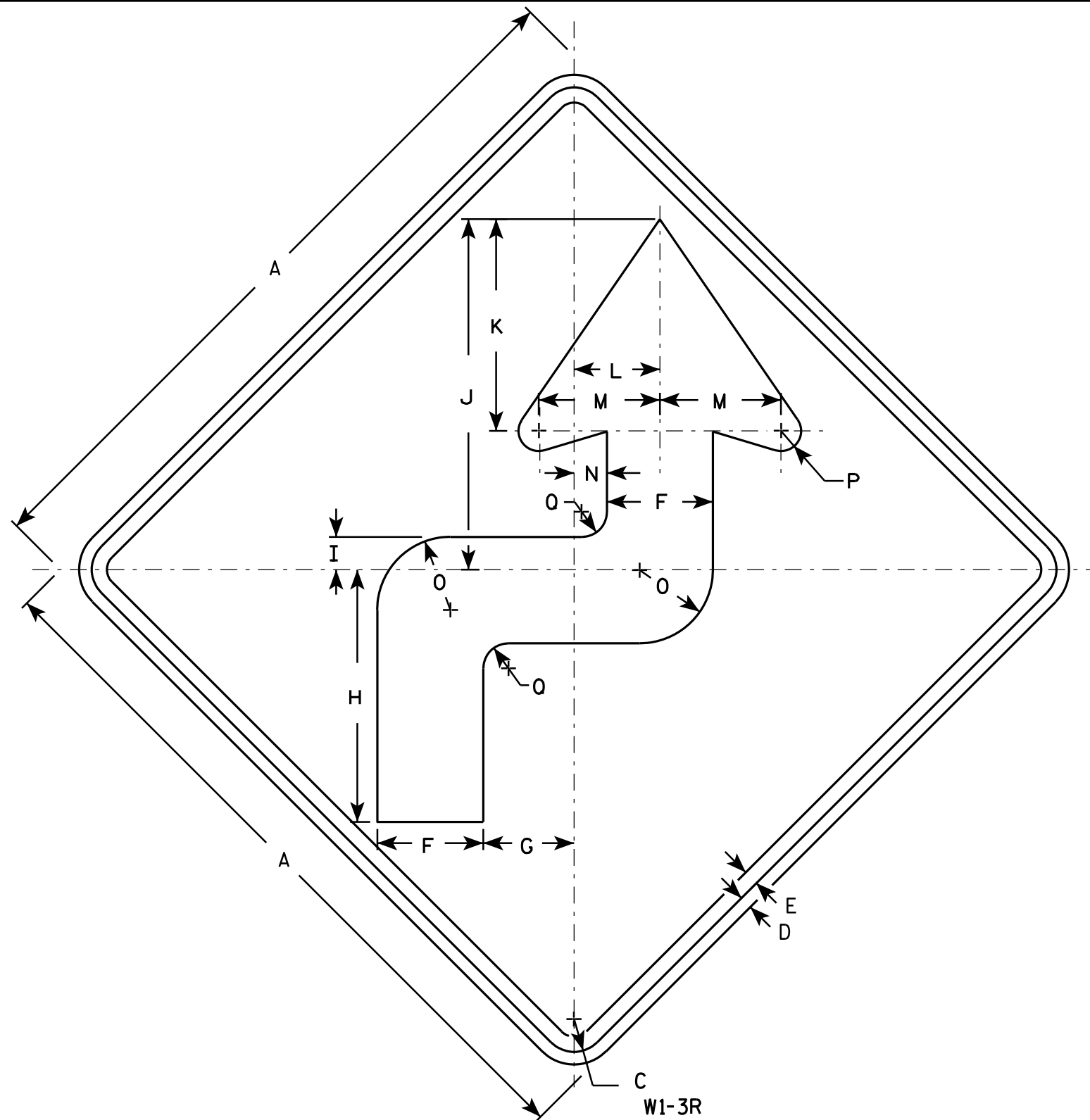
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2		8 1/4	3 1/2	4 1/2	1 3/4	2 3/8	7 1/4	7	4	1/2												4.0
2S	30		1 3/8	1/2	5/8		10 1/4	4 3/8	5 5/8	2 1/4	3	9 1/8	8 3/4	5	5/8												6.25
2M	36		1 5/8	5/8	3/4		12 3/8	5 1/4	6 3/4	2 5/8	3 1/2	10 7/8	10 1/2	6	3/4												9.0
3	36		1 5/8	5/8	3/4		12 3/8	5 1/4	6 3/4	2 5/8	3 1/2	10 7/8	10 1/2	6	3/4												9.0
4	36		1 5/8	5/8	3/4		12 3/8	5 1/4	6 3/4	2 5/8	3 1/2	10 7/8	10 1/2	6	3/4												9.0
5	48		2 1/4	3/4	1		16 1/2	7	9	3 1/2	4 5/8	14 1/2	14	8	1												16.0

STANDARD SIGN  
W1-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
For State Traffic Engineer

DATE 5/15/12 PLATE NO. W1-2.10



**NOTES**

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - Yellow  
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. W1-3L is the same as W1-3R except the arrow is reversed along the vertical centerline.

7

7

W1-3R

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2	3 1/2	3	8 3/8	1 1/8	11 5/8	7	2 3/4	4	1 1/8	2 1/2	5/8	7/8										4.0
2S	36		1 5/8	5/8	3/4	5 1/4	4 1/2	12 1/2	1 5/8	17 3/8	10 1/2	4 1/4	6	1 5/8	3 5/8	1	1 1/4										9.0
2M	36		1 5/8	5/8	3/4	5 1/4	4 1/2	12 1/2	1 5/8	17 3/8	10 1/2	4 1/4	6	1 5/8	3 5/8	1	1 1/4										9.0
3	36		1 5/8	5/8	3/4	5 1/4	4 1/2	12 1/2	1 5/8	17 3/8	10 1/2	4 1/4	6	1 5/8	3 5/8	1	1 1/4										9.0
4	36		1 5/8	5/8	3/4	5 1/4	6	12 1/2	1 5/8	17 3/8	10 1/2	4 1/4	6	1 5/8	3 5/8	1	1 1/4										9.0
5	48		2 1/4	3/4	1	7	6	16 5/8	2 1/4	23 1/4	14	5 5/8	8	2 1/8	4 7/8	1 1/4	1 5/8										16.0

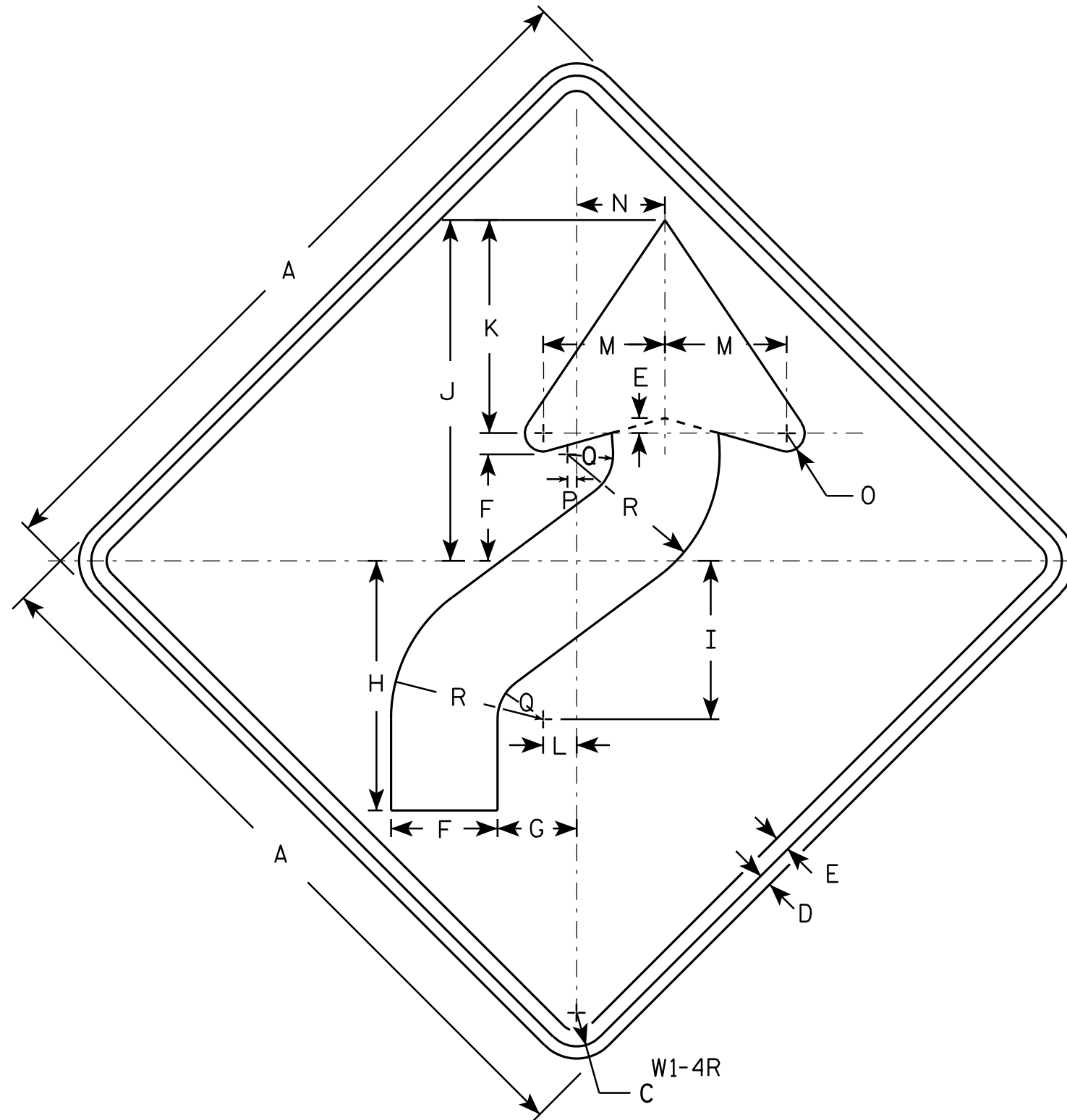
**STANDARD SIGN**  
W1-3

*WISCONSIN DEPT OF TRANSPORTATION*

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 5/17/12 PLATE NO. W1-3.8

PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: **E**



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - Yellow  
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. W1-4L is the same as W1-4R except the arrow is reversed along the vertical centerline.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2	3 1/2	2 5/8	8 1/4	5 1/4	11 1/4	7	1 1/8	4	3	5/8	1/4	1 1/2	5									4.0
2S	30		1 3/8	1/2	5/8	4 3/8	3 1/4	10 1/4	6 1/2	14	8 3/4	1 3/8	5	3 5/8	3/4	3/8	1 7/8	6 1/4									6.25
2M	36		1 5/8	5/8	3/4	5 1/4	4	12 3/8	7 7/8	16 7/8	10 1/2	1 5/8	6	4 1/2	1	1/2	2 1/4	7 1/2									9.0
3	36		1 5/8	5/8	3/4	5 1/4	4	12 3/8	7 7/8	16 7/8	10 1/2	1 5/8	6	4 1/2	1	1/2	2 1/4	7 1/2									9.0
4	36		1 5/8	5/8	3/4	5 1/4	4	12 3/8	7 7/8	16 7/8	10 1/2	1 5/8	6	4 1/2	1	1/2	2 1/4	7 1/2									9.0
5	48		2 1/4	3/4	1	7	5 1/4	16 1/2	10 1/2	22 1/2	14	2 1/4	8	6	1 1/4	5/8	3	10									16.0

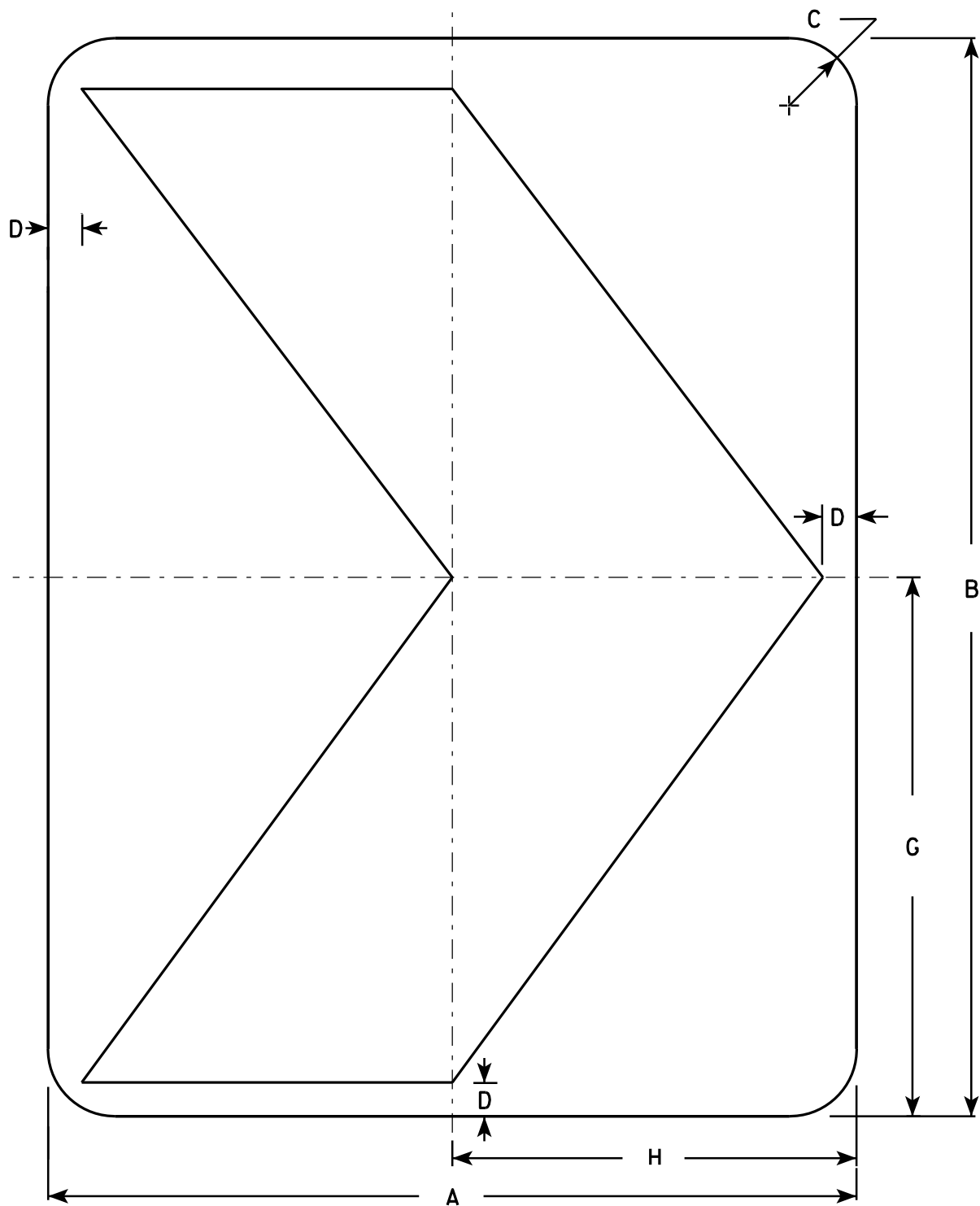
STANDARD SIGN  
W1-4

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Raub*  
for State Traffic Engineer

DATE 5/17/12 PLATE NO. W1-4.11

PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: \_\_\_\_\_ E



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - Yellow  
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

7

7

W1-8

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	12	18	1 1/2	1/2			9	6																			1.5
2S	18	24	1 1/2	3/4			12	9																			3.0
2M	18	24	1 1/2	3/4			12	9																			3.0
3	24	30	1 1/2	1			15	12																			5.0
4	30	36	1 7/8	1 1/4			18	15																			7.5
5	36	48	2 1/4	1 1/2			24	18																			12.0

**STANDARD SIGN**  
W1-8

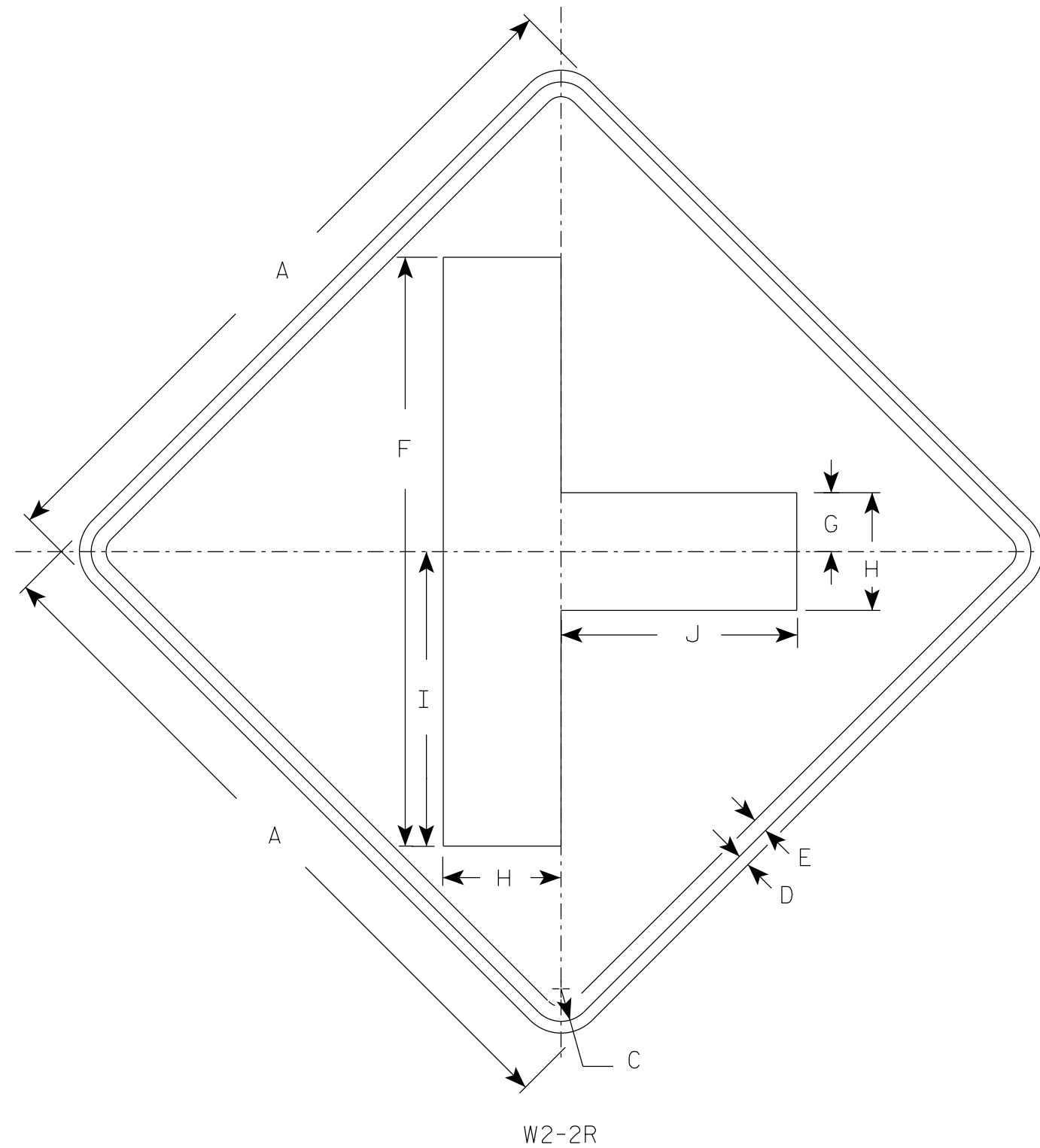
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 6/7/10 PLATE NO. W1-8.6

NOTES

1. Sign is Type II - Type F Reflective
2. Color:  
Background - Yellow  
Message - Black
3. W2-2L same as W2-2R but is rotated 180° when mounted.



W2-2R

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2	20	2	4	10	8																	4.0
2S	30		1 3/8	1/2	5/8	25	2 1/2	5	12 1/2	10																	6.25
2M	30		1 3/8	1/2	5/8	25	2 1/2	5	12 1/2	10																	6.25
3	36		1 5/8	5/8	3/4	30	3	6	15	12																	9.0
4	48		2 1/4	3/4	1	40	4	8	20	16																	16.0
5																											

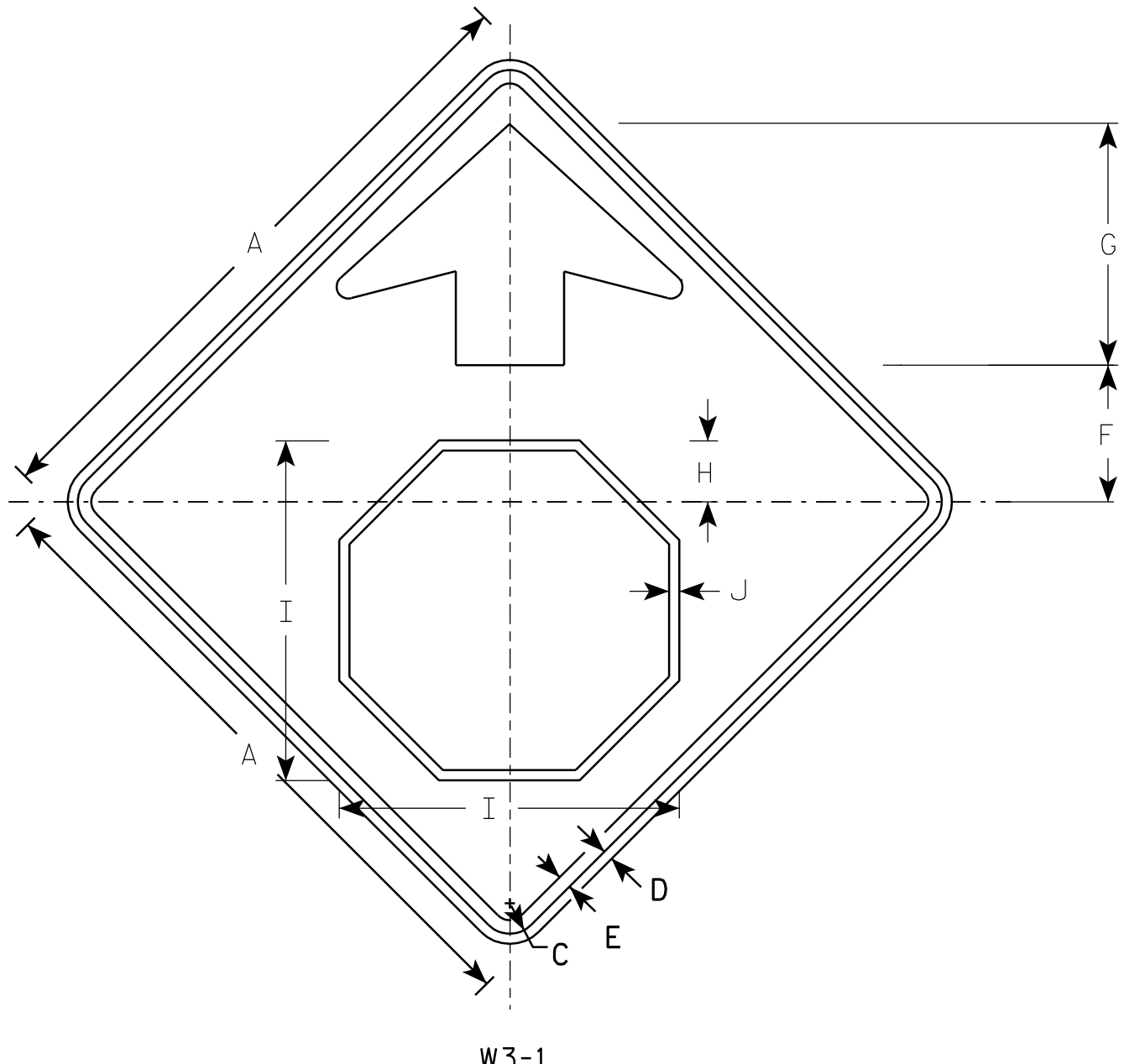
STANDARD SIGN  
W2-2 L&R

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Rauch*  
for State Traffic Engineer

DATE 11/18/2021 PLATE NO. W2-2.7

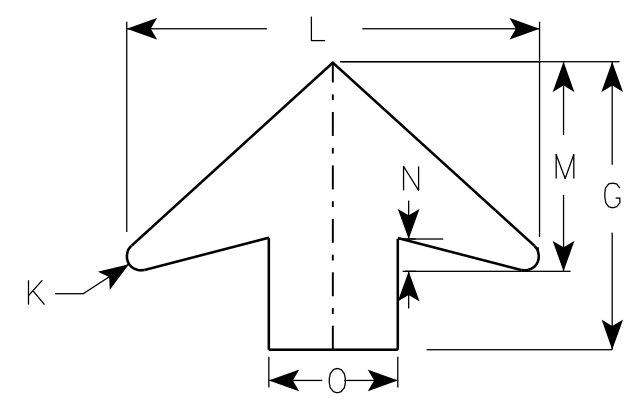
PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: \_\_\_\_\_ **E**



W3-1

NOTES

1. All Signs Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
 Background - YELLOW  
 Arrow & Border - BLACK  
 Stop Symbol - WHITE BORDER ON RED BACKGROUND



ARROW DETAIL

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30		1 3/8	1/2	5/8	6 1/4	11 1/4	2 7/8	15 3/4	1/2	1/2	16	8	1 1/4	5												6.25
2S	36		1 5/8	5/8	3/4	7 1/2	13 1/2	3 1/2	19	5/8	5/8	19 1/4	9 3/4	1 5/8	6												9.0
2M	36		1 5/8	5/8	3/4	7 1/2	13 1/2	3 1/2	19	5/8	5/8	19 1/4	9 3/4	1 5/8	6												9.0
3	36		1 5/8	5/8	3/4	7 1/2	13 1/2	3 1/2	19	5/8	5/8	19 1/4	9 3/4	1 5/8	6												9.0
4	48		2 1/4	3/4	1	10	17 7/8	4 1/2	25 1/8	3/4	7/8	25 5/8	13	2	8												16.0
5	48		2 1/4	3/4	1	10	17 7/8	4 1/2	25 1/8	3/4	7/8	25 5/8	13	2	8												16.0

**STANDARD SIGN**  
W3-1

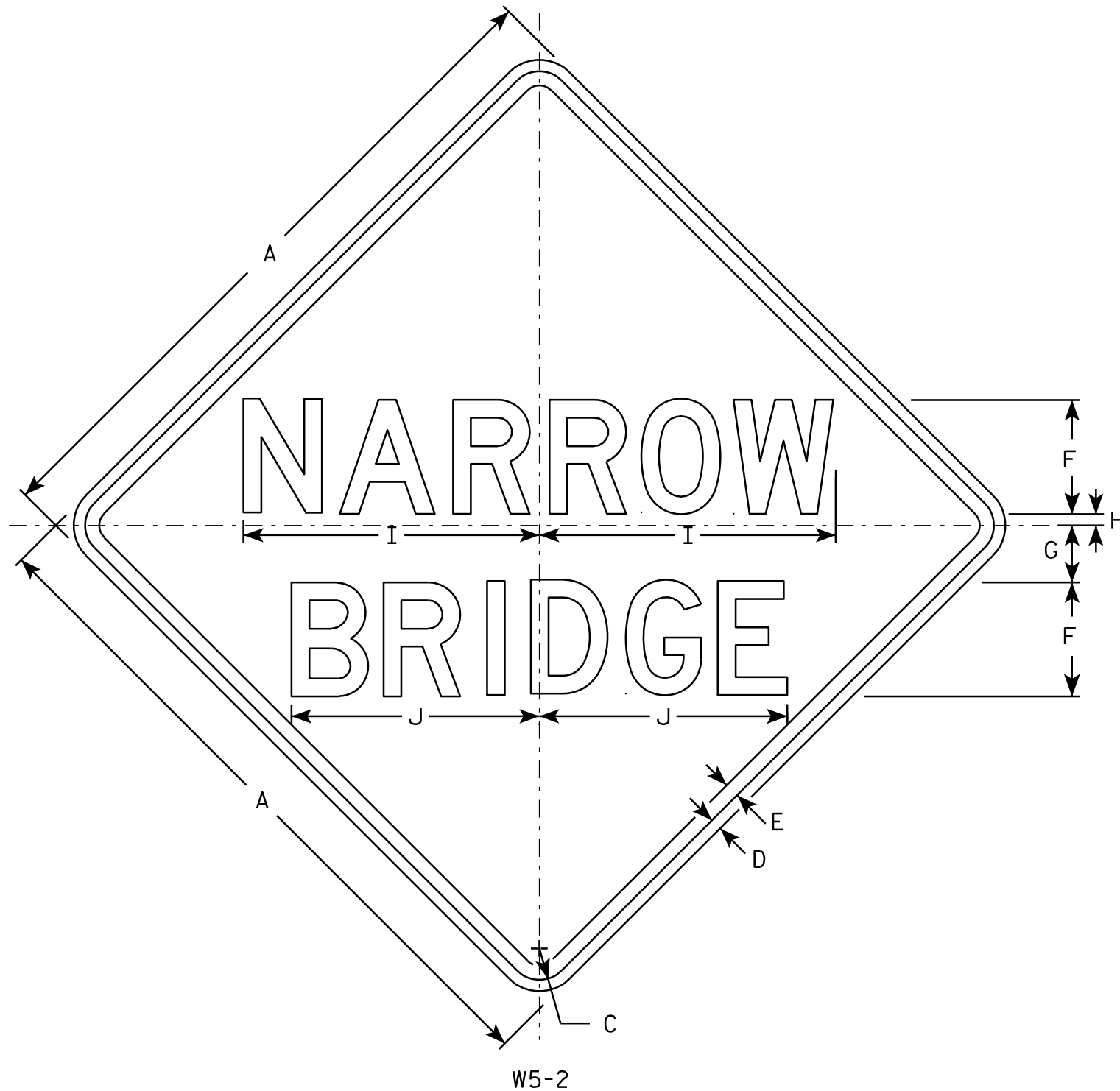
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 6/7/10 PLATE NO. W3-1.12

PROJECT NO: \_\_\_\_\_ SHEET NO: \_\_\_\_\_ E





NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - Yellow  
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

7

7

W5-2

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30		1 3/8	1/2	5/8	5	2 1/2	1/2	13	10 1/8																	6.25
2S	36		1 5/8	5/8	3/4	6	3	3/4	15 5/8	13 1/8																	9.0
2M	36		1 5/8	5/8	3/4	6	3	3/4	15 5/8	13 1/8																	9.0
3	36		1 5/8	5/8	3/4	6	3	3/4	15 5/8	13 1/8																	9.0
4	48		2 1/4	3/4	1	8	4	3/4	20 3/4	17 3/8																	16.0
5																											

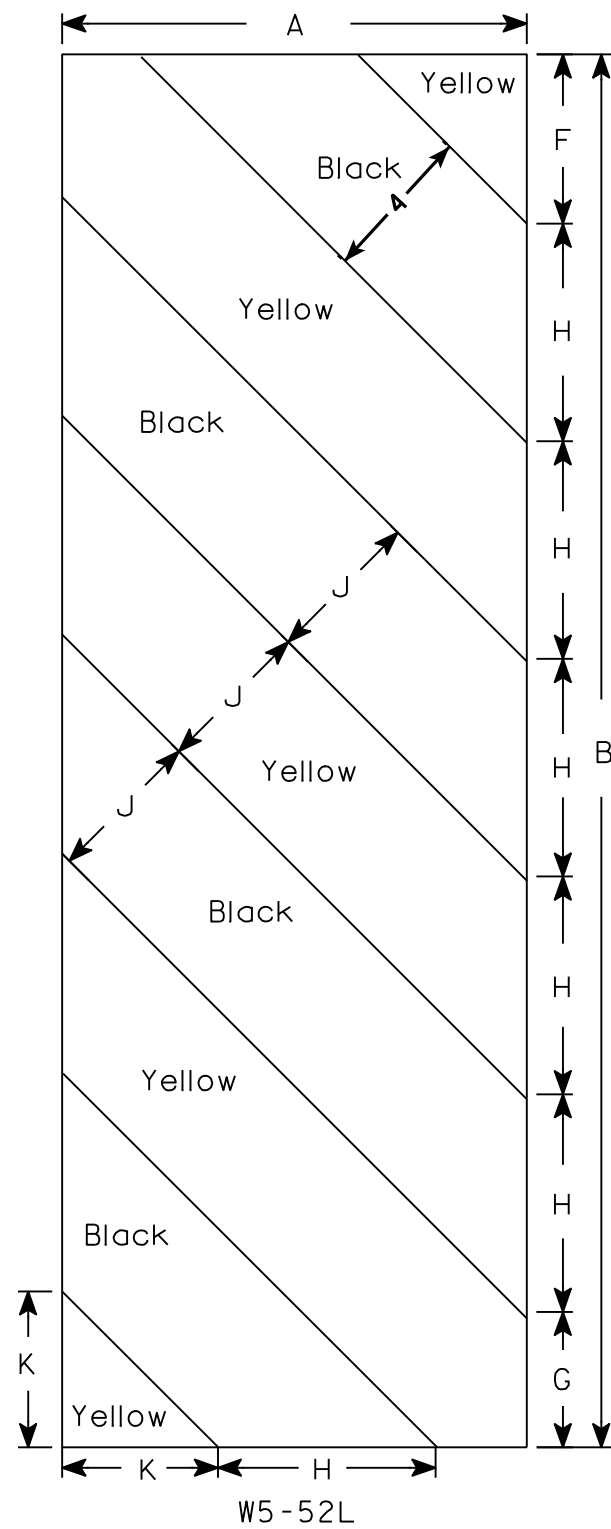
**STANDARD SIGN**  
**W5-2**

*WISCONSIN DEPT OF TRANSPORTATION*

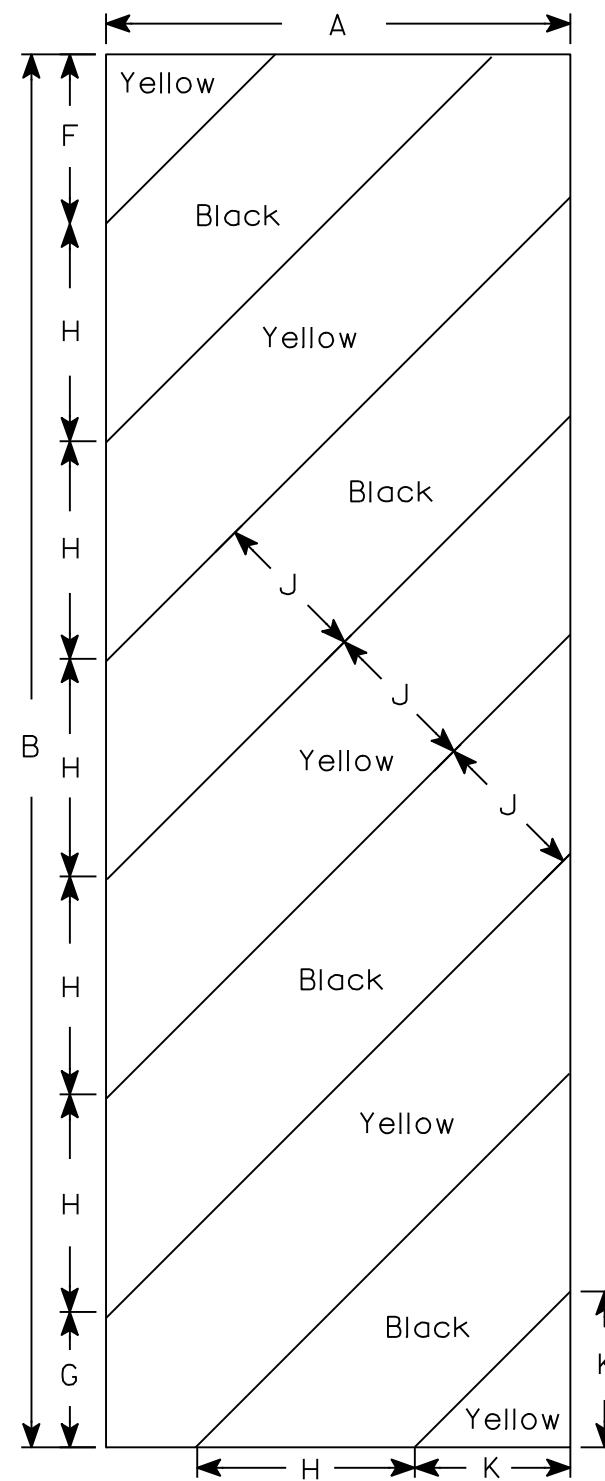
APPROVED *Matthew R. Rauch*  
*for State Traffic Engineer*

DATE 03/12/13 PLATE NO. W5-2.8

PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: **E**



W5-52L



W5-52R

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - Yellow  
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. Alternate colors of stripes as shown.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	12	36				4 3/8	3 1/2	5 5/8	45°	4	4																3.0
2M	12	36				4 3/8	3 1/2	5 5/8	45°	4	4																3.0
3	18	54				6	5 1/2	8 1/2	45°	6	6 9/16																6.75
4																											
5																											

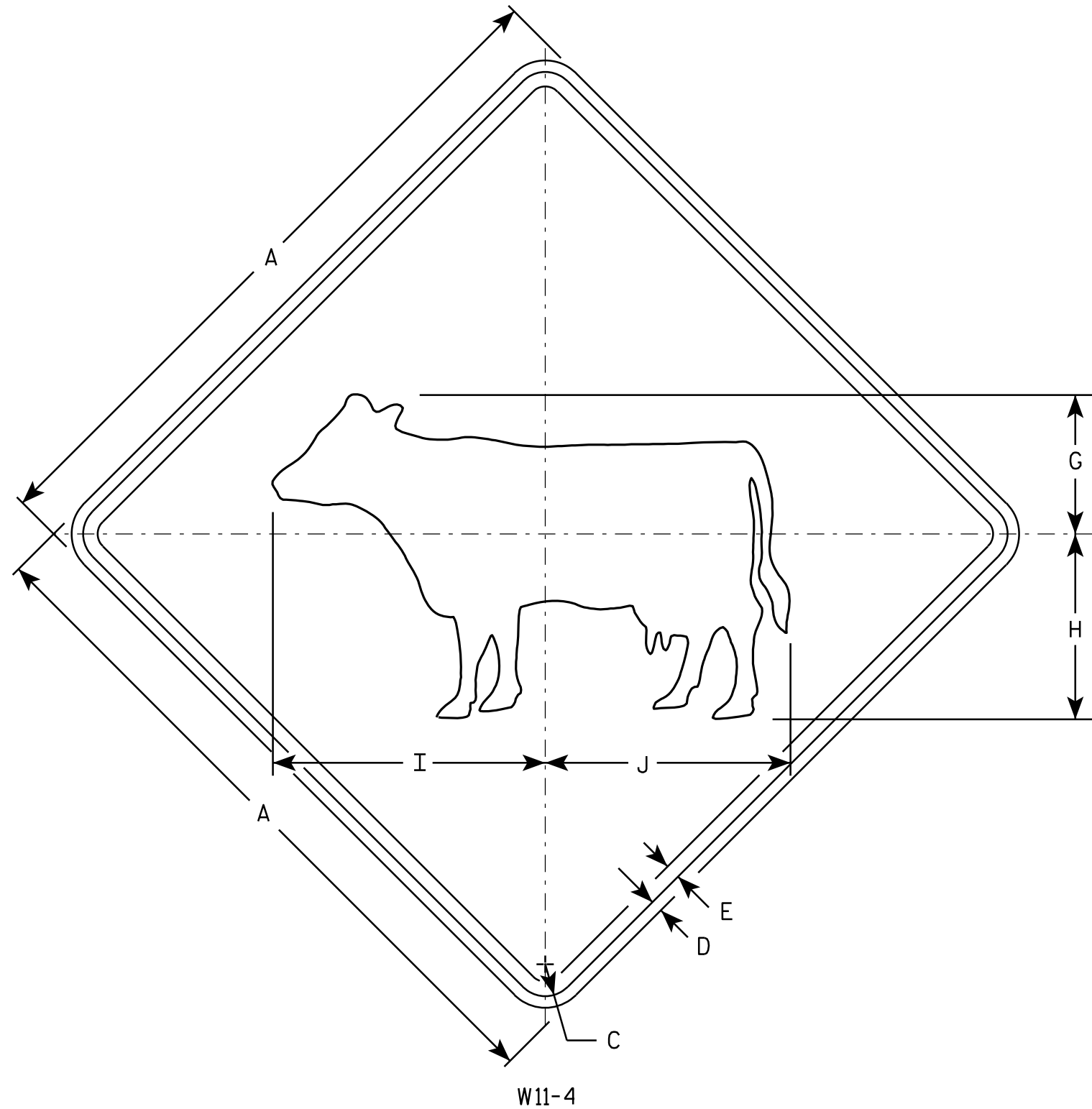
STANDARD SIGN  
W5-52L & W5-52R

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 5/29/12 PLATE NO. W5-52.9

PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: \_\_\_\_\_ E



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - Yellow  
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2		4 3/4	6 3/8	9 1/4	8 3/8																	4.0
2S	30		1 3/8	1/2	5/8		6	8	11 3/4	10 1/2																	6.25
2M	30		1 3/8	1/2	5/8		6	8	11 3/4	10 1/2																	6.25
3	36		1 5/8	5/8	3/4		7 1/4	9 5/8	14 1/8	12 5/8																	9.0
4	36		1 5/8	5/8	3/4		7 1/4	9 5/8	14 1/8	12 5/8																	9.0
5	48		2 1/4	3/4	1		9 1/2	12 5/8	18 1/2	16 5/8																	16.0

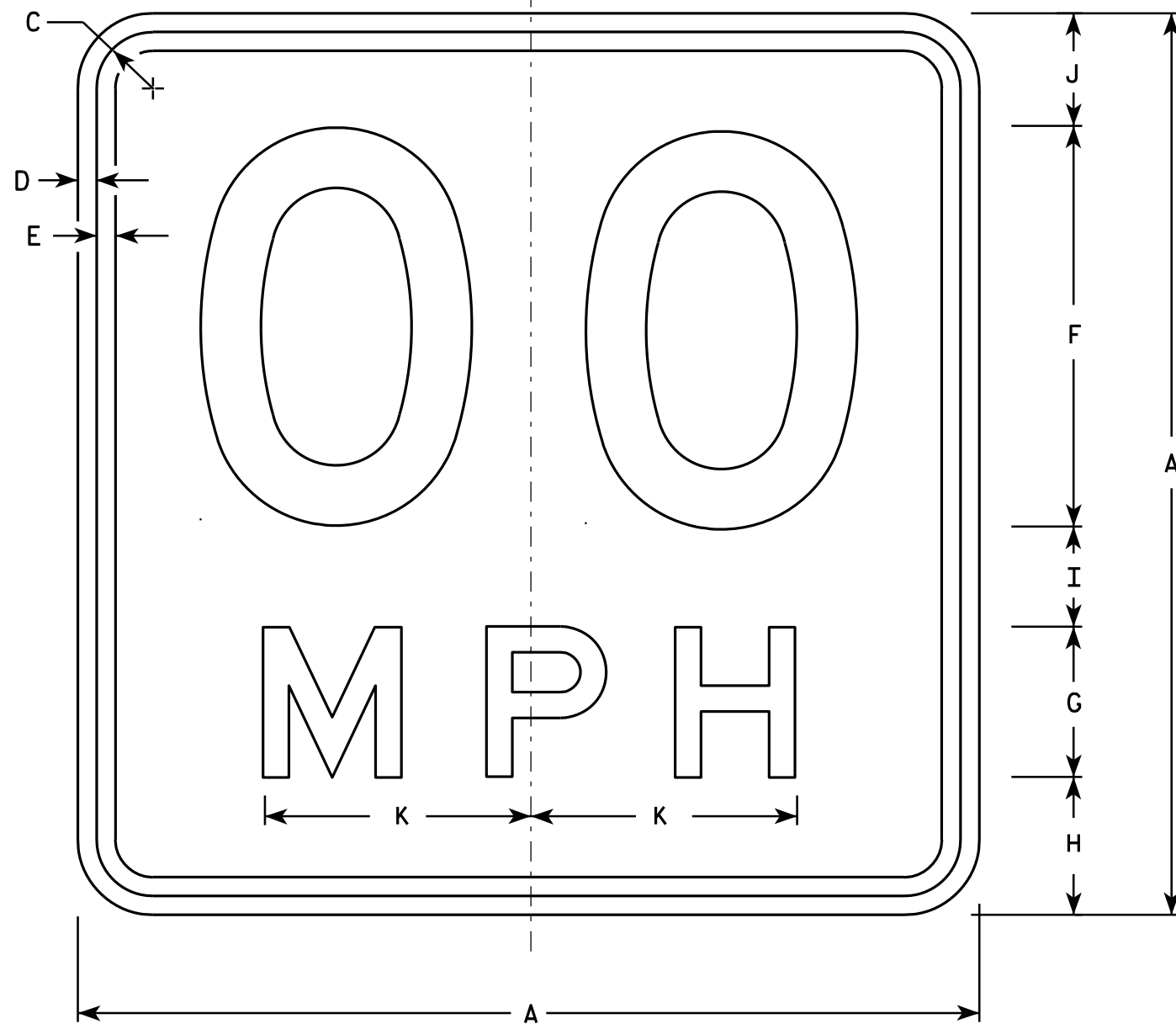
**STANDARD SIGN**  
**W11-4**

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
State Traffic Engineer

DATE 3/13/13 PLATE NO. W11-4.5

PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: **E**



**NOTES**

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - Yellow  
Message - Black
3. Message Series - See Note 6
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals and optically space about centerline to achieve proper balance.
6. Line 1 is Series D  
Line 2 is Series E

W13-1

\* For 30" x 30" Warning Signs, use 18" x 18" W13-1 signs.  
For 36" x 36" Warning Signs, use 24" x 24" W13-1 signs.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18		1 1/8	3/8	3/8	8	3	2 3/4	2	2 1/4	5 3/8																2.25
* 2S	18		1 1/8	3/8	3/8	8	3	2 3/4	2	2 1/4	5 3/8																2.25
* 2M	18		1 1/8	3/8	3/8	8	3	2 3/4	2	2 1/4	5 3/8																2.25
3	24		1 1/8	3/8	1/2	10	4	4	2 3/4	3 1/4	6 5/8																4.00
4	36		1 5/8	5/8	3/4	16	6	5 1/2	4	4 1/2	10 5/8																9.00
5	36		1 5/8	5/8	3/4	16	6	5 1/2	4	4 1/2	10 5/8																9.00

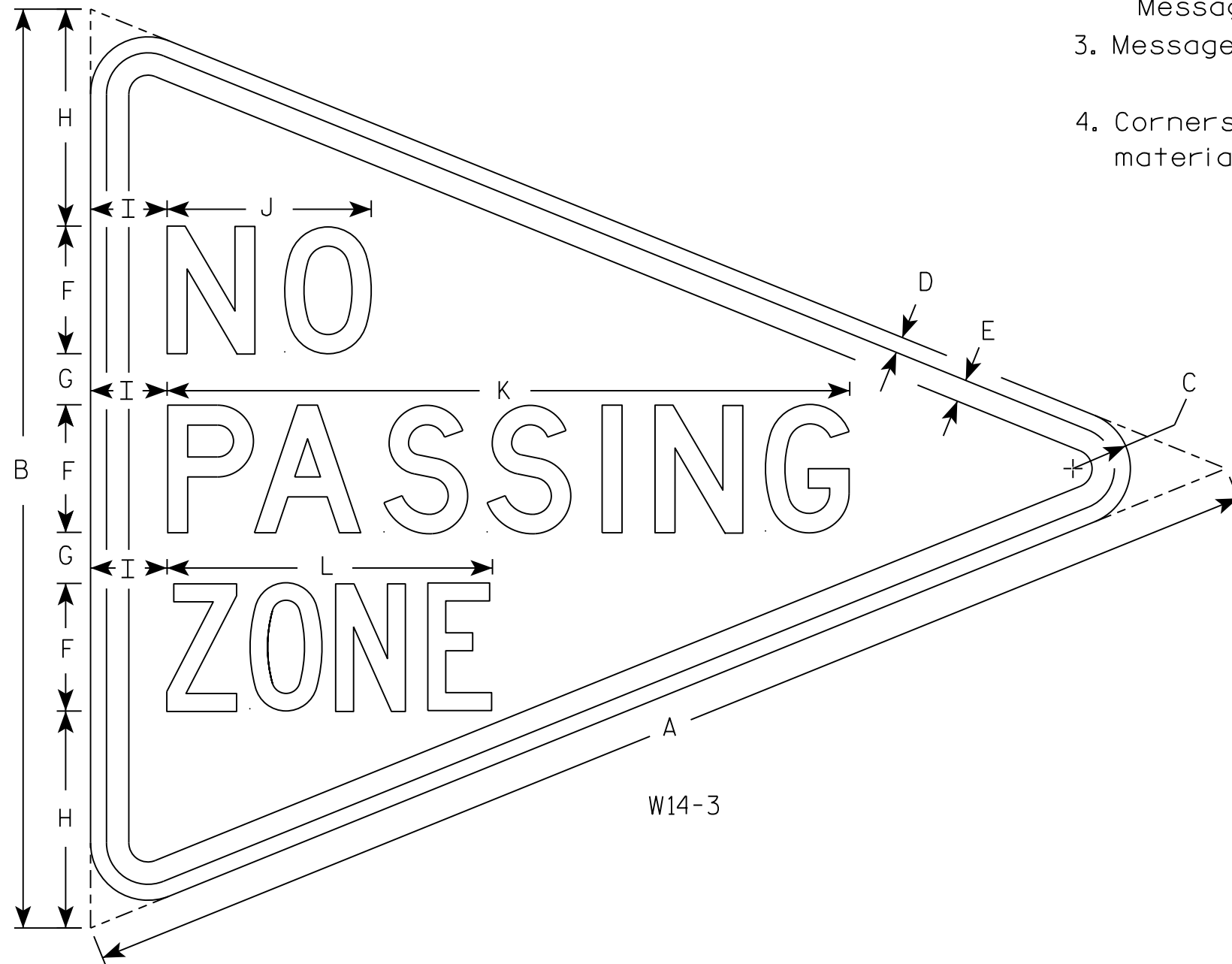
STANDARD SIGN  
W13-1

WISCONSIN DEPT OF TRANSPORTATION  
APPROVED *Matthew R. Rauch*  
For State Traffic Engineer

DATE 5/31/12 PLATE NO. W13-1.16

NOTES

1. Sign is Type II - Type F Reflective
2. Color:  
Background - Yellow  
Message - Black
3. Message Series - Lines 1 and 2 are Series D.  
Line 3 is series C.
4. Corners and borders shall be rounded on all base materials for this sign.



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48	36	2 1/4	5/8	7/8	5	2	8 1/2	3	8	26 3/4	12 3/4															5.56
2M																											
3																											
4																											
5																											

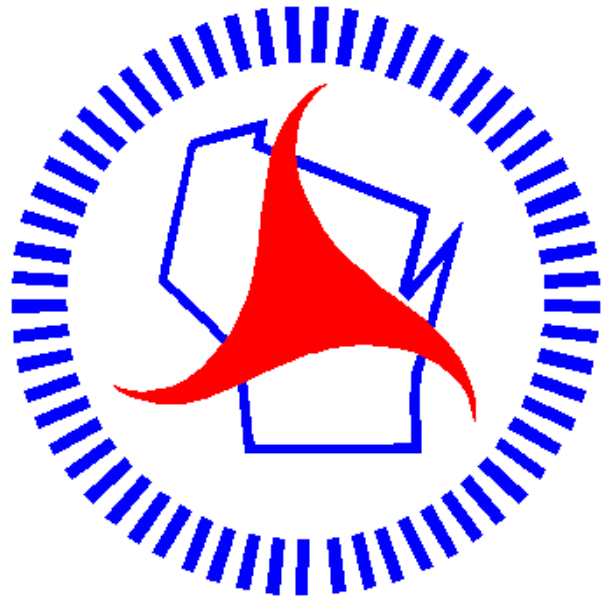
STANDARD SIGN  
W14-3

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 3/21/17 PLATE NO. W14-3.10

PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: **E**



## ***Wisconsin Department of Transportation***

Dedicated people creating transportation solutions through innovation and exceptional service.

<http://www.dot.wisconsin.gov>

ORDER OF SHEETS

Section No.	1	Title
Section No.	2	Typical Sections and Details
Section No.	3	Estimate of Quantities
Section No.	3	Miscellaneous Quantities
Section No.	4	Right of Way Plan
Section No.	5	Plan and Profile
Section No.	6	Standard Detail Drawings
Section No.	7	Sign Plates
Section No.	8	Structure Plans
Section No.	9	Computer Earthwork Data
Section No.	9	Cross Sections

TOTAL SHEETS = 100



DESIGN DESIGNATION

A.A.D.T.	2022	=	230
A.A.D.T.	2041	=	250
D.H.V.		=	25
D.D.		=	50/50
T.		=	18%
DESIGN SPEED		=	55 MPH
ESALS		=	63,000

CONVENTIONAL SYMBOLS

PLAN	PROFILE
CORPORATE LIMITS	GRADE LINE
PROPERTY LINE	ORIGINAL GROUND
LOT LINE	MARSH OR ROCK PROFILE (To be noted as such)
LIMITED HIGHWAY EASEMENT	SPECIAL DITCH
EXISTING RIGHT OF WAY	GRADE ELEVATION
PROPOSED OR NEW R/W LINE	CULVERT (Profile View)
SLOPE INTERCEPT	UTILITIES
REFERENCE LINE	ELECTRIC
EXISTING CULVERT	FIBER OPTIC
PROPOSED CULVERT (Box or Pipe)	GAS
COMBUSTIBLE FLUIDS	SANITARY SEWER
MARSH AREA	STORM SEWER
WOODED OR SHRUB AREA	TELEPHONE
	WATER
	UTILITY PEDESTAL
	POWER POLE
	TELEPHONE POLE

# STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

## PLAN OF PROPOSED IMPROVEMENT

### WOODLAND - WONEWOC (CTH G)

CTH EE TO JUNEAU COUNTY LINE

CTH G

SAUK COUNTY

STATE PROJECT NUMBER

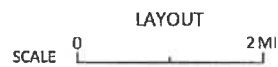
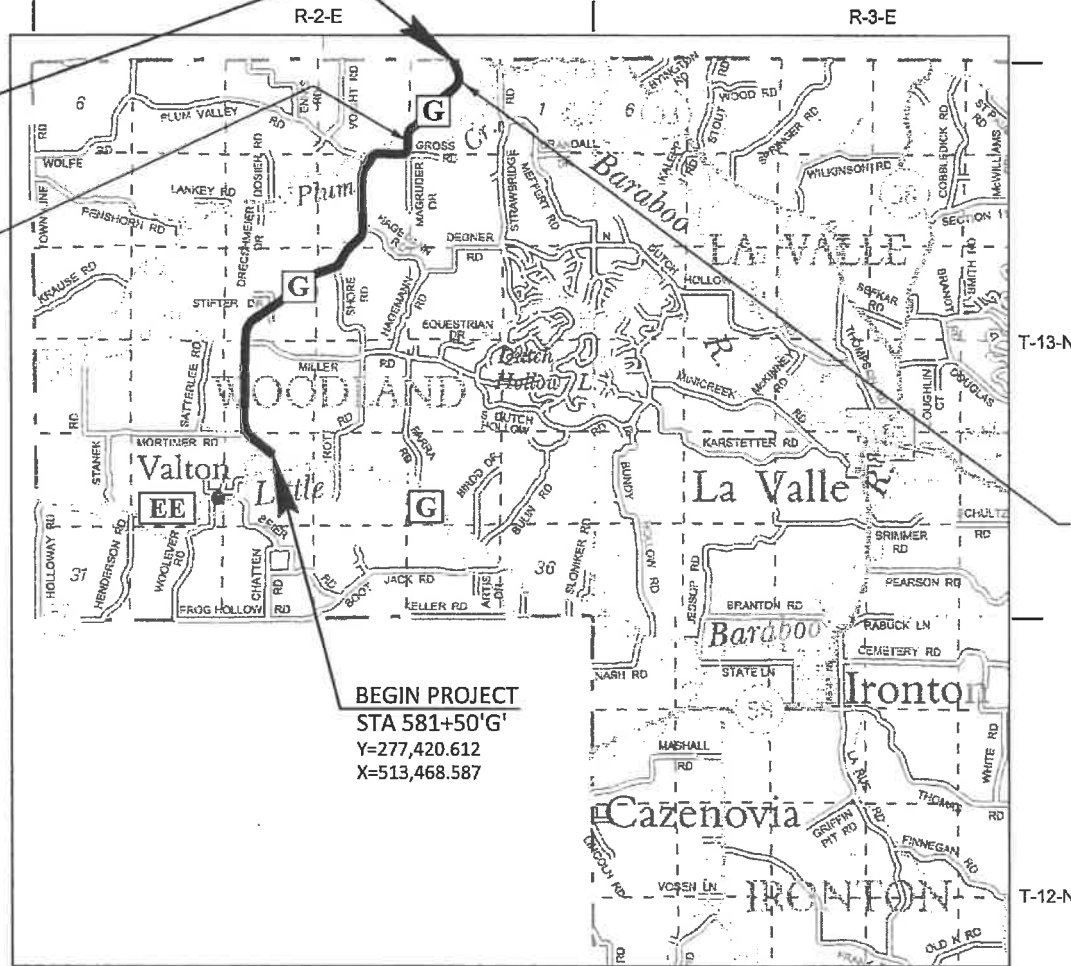
5908-00-75

END PROJECT  
STA 879+75'G'

EXCEPTION TO NET CENTERLINE LENGTH  
STA 816+59.02'G' - 816+93.31'G'  
(STRUCTURE B-56-157)

BEGIN PROJECT  
STA 581+50'G'  
Y=277,420.612  
X=513,468.587

EXCEPTION TO NET CENTERLINE LENGTH  
STA 864+82.94'G' TO 866+56.61'G'  
(STRUCTURE B-56-202)



TOTAL NET LENGTH OF CENTERLINE = 5.609 MI

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS), SAUK COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES.

ELEVATIONS ARE REFERENCED TO NAVD 88 (2012). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 12A.

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
5908-00-75	WISC 2023066	1

ACCEPTED FOR

COUNTY of SAUK

7/20/2022  
(Date)

Highway Commissioner  
(Signature & Title of Official)

ORIGINAL PLANS PREPARED BY



7/20/2022  
(Date)

(Signature)

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

PREPARED BY	Surveyor	SEH
Designer	SEH	
Project Manager	DELLA KOENIG P.E.	
Regional Examiner	REGION	
Regional Supervisor	KYLE HEMP P.E.	

APPROVED FOR THE DEPARTMENT

DATE: 7/26/2022

(Signature)

E

**STANDARD ABBREVIATIONS**

ABUT	ABUTMENT	ID	INSIDE DIAMETER
AC	ACRE	INV	INVERT
AGG	AGGREGATE	IP	IRON PIPE ON PIN
AECPRC	APRON ENDWALL FOR CULVERT PIPE REINFORCED CONCRETE	LHF	LEFT-HAND FORWARD
AECPCS	APRON ENDWALL FOR CULVERT PIPE CORRUGATED STEEL	L	LENGTH OF CURVE
ASPH	ASPHALTIC	LF	LINEAR FOOT
AVG	AVERAGE	LC	LONG CHORD OF CURVE
ADT	AVERAGE DAILY TRAFFIC	LS	LUMP SUM
BF	BACK FACE	MH	MANHOLE
BM	BENCH MARK	MOR	MID POINT OF RADIUS
BR	BRIDGE	NC	NORMAL CROWN
CE	COMMERCIAL ENTRANCE	NO	NUMBER
C/L	CENTER LINE	OBLIT	OBLITERATE
Δ	CENTRAL ANGLE OR DELTA	PAVT	PAVEMENT
COB	CENTER OF BARRIER	PE	PRIVATE ENTRANCE
CONC	CONCRETE	PVRC	POINT OF VERTICAL REVERSE CURVE
CPRC	CULVERT PIPE REINFORCED CONCRETE	QOR	QUARTER POINT OF RADIUS
CPRCHE	CULVERT PIPE REINFORCED CONCRETE HORIZONTAL ELLIPTICAL	R	RADIUS
CR	CREEK	REQ'D	REQUIRED
CY	CUBIC YARD	RES	RESIDENCE OR RESIDENTIAL
C&G	CURB AND GUTTER	RHF	RIGHT-HAND FORWARD
D	DEGREE OF CURVE	R/W	RIGHT-OF-WAY
DHV	DESIGN HOUR VOLUME	R	RIVER
DISCH	DISCHARGE	RDWY	ROADWAY
DG	DITCH GRADE	R/L	REFERENCE LINE
DWY	DRIVEWAY	SALV	SALVAGED
X	EAST GRID COORDINATE	SAN	SANITARY SEWER
EAT	STEEL PLATE BEAM GUARD ENERGY ABSORBING TERMINAL	SF	SQUARE FEET
EOR	END POINT OF RADIUS	SY	SQUARE YARD
EL	ELEVATION	SDD	STANDARD DETAIL DRAWINGS
ENT	ENTRANCE	STA	STATION
ESALS	EQUIVALENT SINGLE AXLE LOADS	SS	STORM SEWER
EXC	EXCAVATION	SSPRC	STORM SEWER PIPE REINFORCED CONCRETE
EBS	EXCAVATION BELOW SUBGRADE	SE	SUPERELEVATION RATE
EXIST	EXISTING	TC	TOP OF CURB
FC	FACE OF CURB	T OR TN	TOWN
FF	FACE TO FACE	T	TRUCKS (PERCENT OF)
FERT	FERTILIZE	TYP	TYPICAL
FE	FIELD ENTRANCE	VAR	VARIABLE
FL	FLOW LINE	VC	VERTICAL CURVE
FO	FIBER OPTIC	Y	NORTH GRID COORDINATE
CWT	HUNDREDWEIGHT	YD	YARD
HYD	HYDRANT		

**DNR AREA LIAISON:**

WI DEPT OF NATURAL RESOURCES  
DNR SERVICE CENTER  
3550 MORMON COULEE RD  
LA CROSSE, WI 54601  
TELEPHONE: 608.785.9115  
ATTENTION: KAREN KALVELAGE  
EMAIL: KAREN.KALVELAGE@WISCONSIN.GOV

**WISDOT CONTACT:**

WISCONSIN DEPT OF TRANSPORTATION  
SOUTHWEST REGION  
2101 WRIGHT STREET  
MADISON, WI 53704  
TELEPHONE: 608.246.7963  
ATTENTION: DELLA KOENIG  
EMAIL: DELLA.KOENIG@DOT.WI.GOV

**DESIGN CONTACT:**

SEH INC.  
329 JAY STREET, SUITE 301  
LA CROSSE, WI 54601  
TELEPHONE: 608.498.4804  
ATTENTION: ALEIGHA BURG  
EMAIL: ABURG@SEHINC.COM

**SAUK COUNTY:**

SAUK COUNTY HIGHWAY COMMISSIONER  
620 LINN STREET  
PO BOX 26  
WEST BARABOO, WI 53913  
TELEPHONE: 608.355.4855  
ATTENTION: PAT GAVINSKI  
EMAIL: PATRICK.GAVINSKI@SAUKCOUNTYWI.GOV

**UTILITY CONTACT LIST:**

DAIRYLAND POWER COOPERATIVE - ELECTRICITY  
3200 EAST AVE S  
LACROSSE, WI 54602  
ATTENTION: MIKE LYDON  
TELEPHONE: 608.787.1381  
EMAIL: MIKE.LYDON@DAIRYLANDPOWER.COM

LUMEN - COMMUNICATION LINE  
130 FOURTH STREET  
BARABOO, WI 53913  
ATTENTION: BRIAN STELPLUGH  
TELEPHONE: 608.615.4138  
EMAIL: BRIAN.STELPLUGH@LUMEN.COM

HILLSBORO TELEPHONE - COMMUNICATION LINE  
121 MILL STREET  
HILLSBORO, WI 54634  
ATTENTION: DON HAMMER  
TELEPHONE: 608.489.3230  
EMAIL: DJHAMMER@HILLSBOROTEL.COM

VERNON ELECTRIC - ELECTRICITY  
110 SAUGSTAD ROAD  
WESTBY, WI 54667  
ATTENTION: COLE CARY  
TELEPHONE: 608.634.7472  
EMAIL: CCARY@VERNONELECTRIC.COM

LA VALLE TELEPHONE - COMMUNICATION LINE  
S1421 STATE HIGHWAY 33  
PO BOX 28  
LA VALLE, WI 53941  
ATTENTION: JOSH LIEN  
TELEPHONE: 608.985.7201  
EMAIL: JOSHL@RGTC.COOP

**RUNOFF COEFFICIENT TABLE**

	HYDROLOGIC SOIL GROUP											
	A			B			C			D		
	SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)		
LAND USE:	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER
ROW CROPS	.08	.16	.22	.12	.20	.27	.15	.24	.33	.19	.28	.38
	.22	.30	.38	.26	.34	.44	.30	.37	.50	.34	.41	.56
MEDIAN STRIP-TURF	.19	.20	.24	.19	.22	.26	.20	.23	.30	.20	.25	.30
	.24	.26	.30	.25	.28	.33	.26	.30	.37	.27	.32	.40
SIDE SLOPE-TURF			.25			.27			.28			.30
			.32			.34			.36			.38
PAVEMENT:												
ASPHALT	.70 - .95											
CONCRETE	.80 - .95											
BRICK	.70 - .80											
DRIVES, WALKS	.75 - .85											
ROOFS	.75 - .95											
GRAVEL ROADS, SHOULDERS	.40 - .60											

TOTAL PROJECT AREA = 45.6 ACRES  
TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = 0.0 ACRES

**DIGGERS HOTLINE**  
Dial 811 or (800)242-8511  
www.DiggersHotline.com

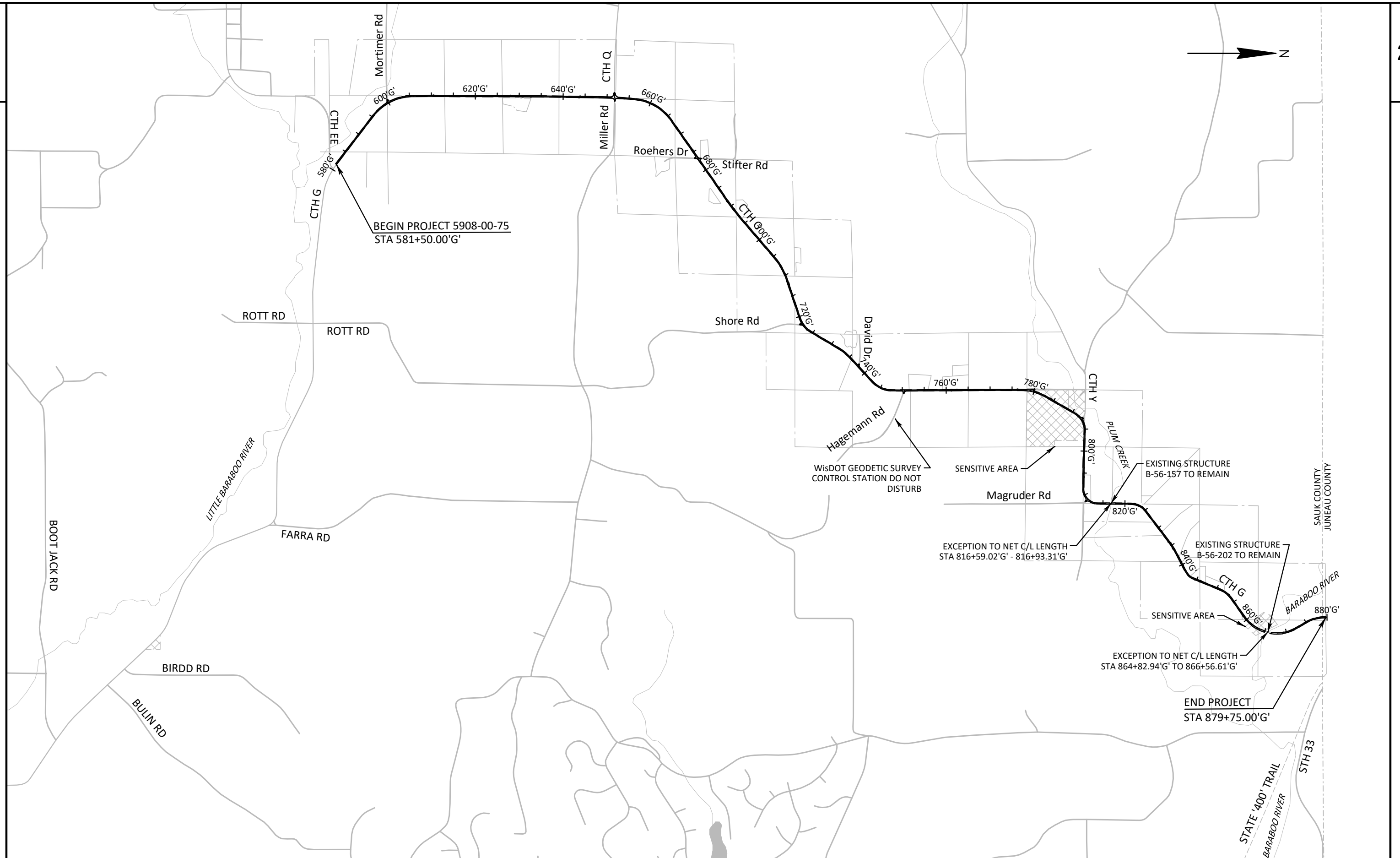
**GENERAL NOTES:**

- NO TREES OR SHRUBS SHALL BE REMOVED WITHOUT APPROVAL OF THE ENGINEER.
- THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.
- THE CONTRACTOR SHALL NOTIFY DIGGERS HOTLINE AND AFFECTED UTILITIES PRIOR TO THE START OF WORK. ANY LOCAL MUNICIPAL UTILITY WHICH IS NOT A MEMBER OF THE DIGGERS HOTLINE MUST BE CONTACTED SEPARATELY.
- WETLANDS, WATERWAYS, AND OTHER ENVIRONMENTALLY SENSITIVE AREAS SHALL BE PROTECTED AT ALL TIMES. DO NOT STORE EQUIPMENT OR MATERIALS NEAR THESE SITES UNLESS APPROVED BY THE ENGINEER.
- TRAFFIC CONTROL DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.
- ASPHALTIC AND CONCRETE SURFACES SHALL BE SAWCUT AT THE MATCH LINE AS SHOWN ON THE PLAN OR AS DIRECTED BY THE ENGINEER.
- CONTRACTOR WILL BE RESPONSIBLE FOR RESHAPING AND SEEDING ANY PREVIOUSLY GRASSED AREAS WHICH ARE DISTURBED BY HIS OPERATION OUTSIDE OF THE NORMAL CONSTRUCTION LIMITS.
- FERTILIZER SHALL NOT BE USED NEAR NAVIGABLE WATERWAYS OR WETLANDS.
- APPLY TACK COAT AT A RATE OF 0.07 GAL/SY TO MILLED SURFACE AND 0.05 GA/SY BETWEEN LAYERS OF HMA PAVEMENT.
- HMA PAVEMENT WEIGHT CALCULATIONS ARE BASED ON 112 LB/SY/IN.
- THE CONTRACTOR'S PAVING OPERATIONS SHALL BE CONSISTENT WITH THE PLAN AND TYPICAL SECTIONS AND CONSTRUCTED TO PREVENT HMA LONGITUDINAL JOINTS FROM BEING LOCATED WITHIN A DRIVING, TURNING, PASSING OR PARKING LANE.

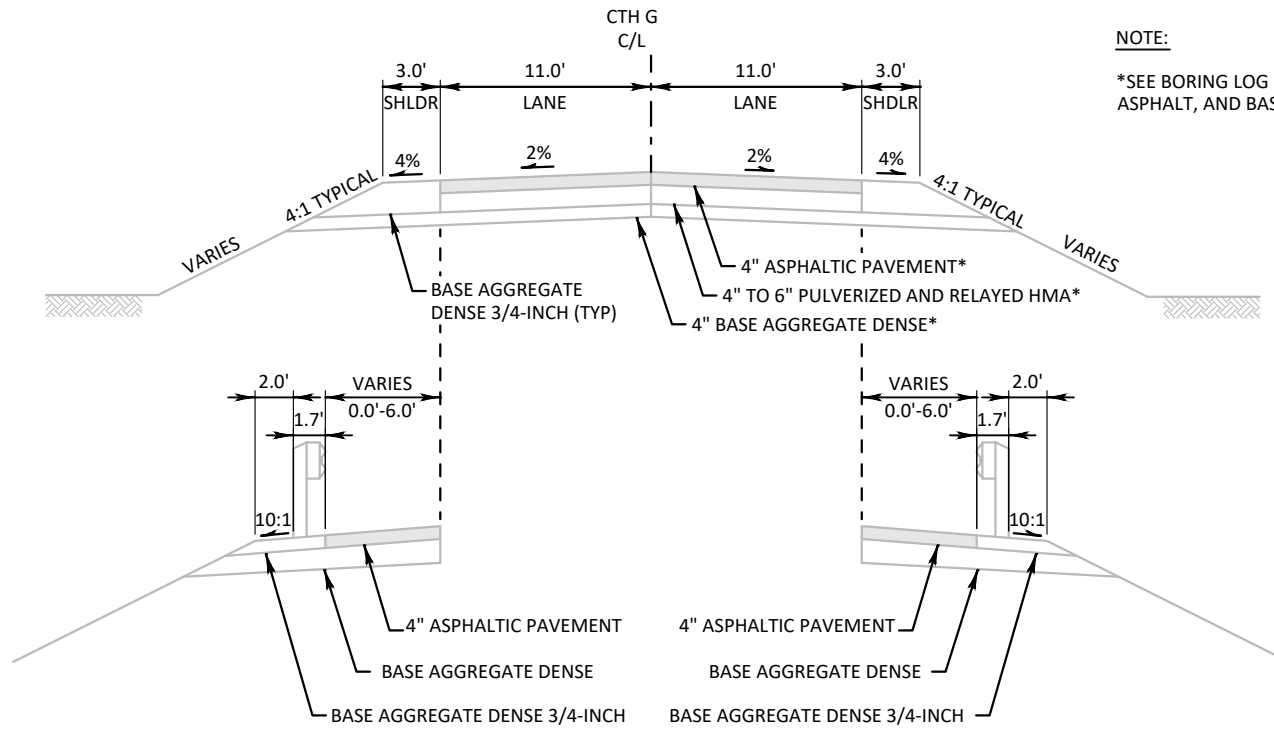
**BORING LOG:**

BORING NO.	STATION	ASPHALTIC PAVEMENT DEPTH (IN)	PULVERIZED HMA (IN)	BASE DEPTH (IN)	SUBBASE MATERIAL
B-7	600+35	4	6	4	Sand
B-8	654+70	4	6	4	Sand
B-9	714+17	4	4	4	Sand





PROJECT NO: 5908-00-75	HWY: CTH G	COUNTY: SAUK	PROJECT OVERVIEW	SHEET E
------------------------	------------	--------------	------------------	---------



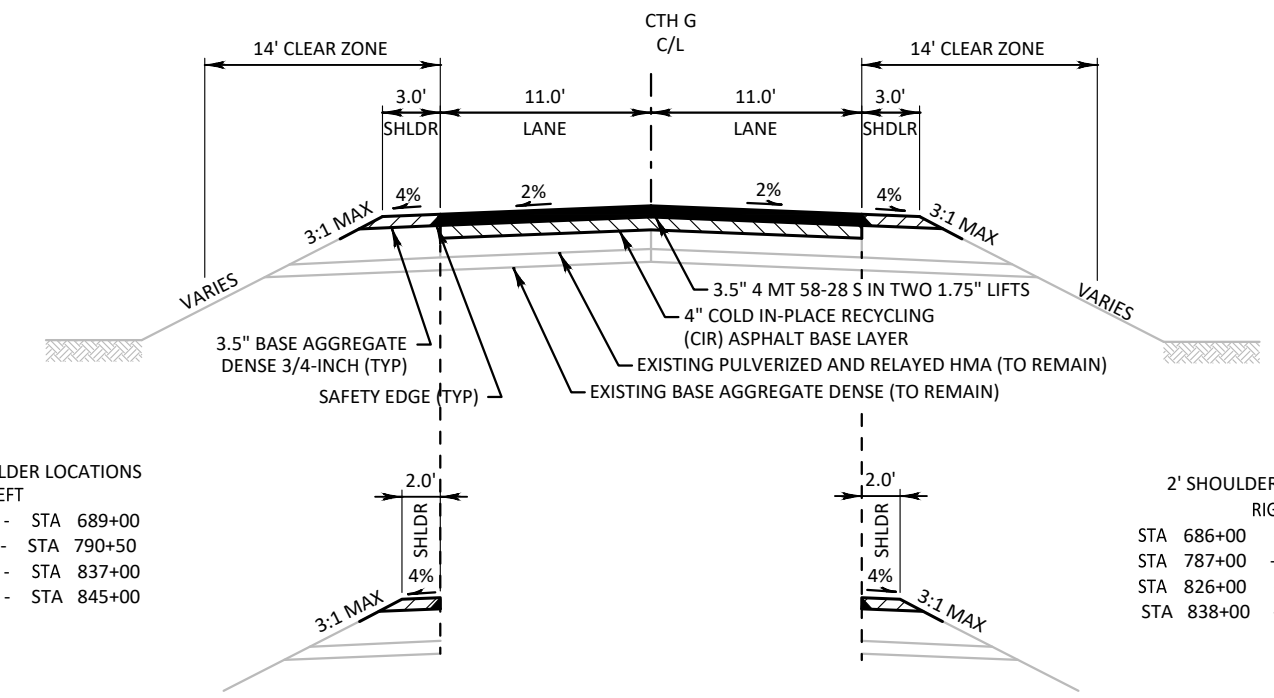
NOTE:  
\*SEE BORING LOG FOR ASPHALT, RECYCLED ASPHALT, AND BASE DEPTHS.

GUARD RAIL LOCATIONS  
LEFT  
STA 712+99 - STA 713+15  
STA 815+77 - STA 817+76  
STA 864+90 - STA 865+16  
STA 866+48 - STA 867+70

GUARD RAIL LOCATIONS  
RIGHT  
STA 712+77 - STA 712+92  
STA 815+77 - STA 817+76  
STA 861+82 - STA 864+98  
STA 866+21 - STA 869+11

TYPICAL EXISTING SECTION

CTH G  
STA 581+50 - STA 879+75



2' SHOULDER LOCATIONS  
LEFT  
STA 686+00 - STA 689+00  
STA 788+50 - STA 790+50  
STA 835+00 - STA 837+00  
STA 844+00 - STA 845+00

2' SHOULDER LOCATIONS  
RIGHT  
STA 686+00 - STA 692+00  
STA 787+00 - STA 790+50  
STA 826+00 - STA 833+50  
STA 838+00 - STA 842+00

TYPICAL FINISHED SECTION

CTH G  
STA 581+50 - STA 879+75

PROJECT NO:

5908-00-75

HWY: CTH G

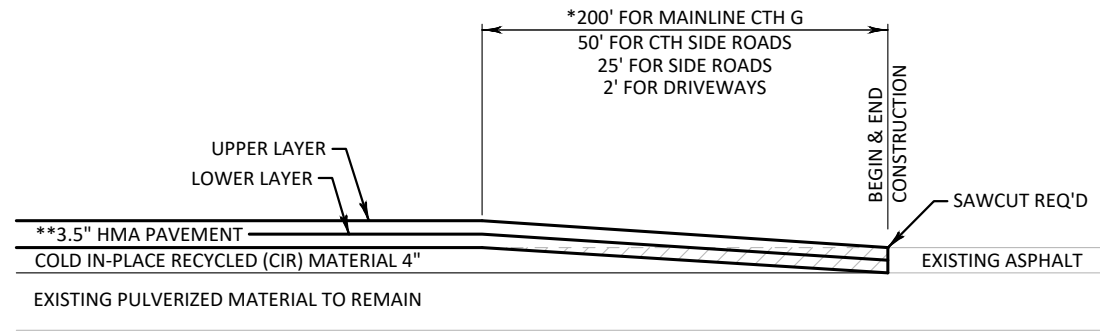
COUNTY: SAUK

TYPICAL SECTIONS

LAYOUT NAME:

SHEET

E



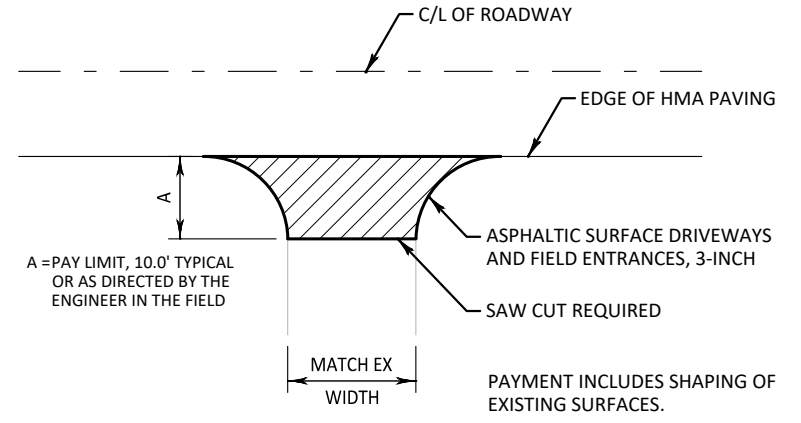
REMOVING ASPHALTIC SURFACE BUTT JOINTS

MAINLINE	**SIDE ROADS
STA 814+59 - STA 816+59 (B-56-157)	MORTIMER RD      DAVID DR
STA 816+93 - STA 818+93 (B-56-157)	CTH Q              HAGEMANN RD
STA 861+73 - STA 864+83 (B-56-202)*	MILLER RD        TOWN SHED RD
STA 866+57 - STA 869+32 (B-56-202)*	ROEHRS DR        CTH Y
STA 877+75 - STA 879+75	STIFTER RD        MAGRUDER RD
	SHORE RD

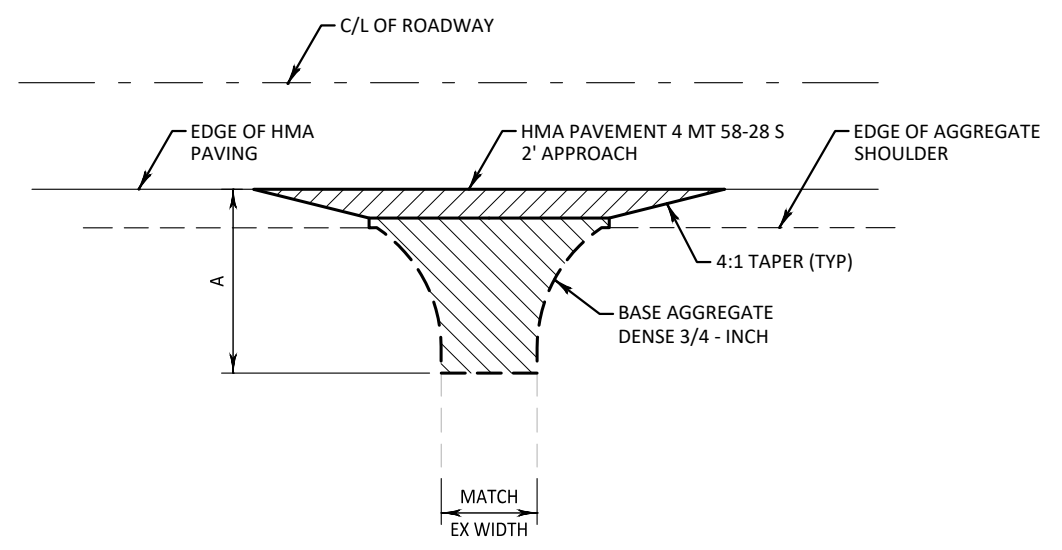
\*BUTT JOINTS AT B-56-202 TO MATCH BEAMGUARD LENGTH:  
310' SOUTH OF BRIDGE, 275' NORTH OF BRIDGE

\*\*3 MT 58-28, 3-INCH, FOR SIDE ROADS

REMOVING ASPHALTIC SURFACE BUTT JOINT



ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES (PE, CE, OR FE)



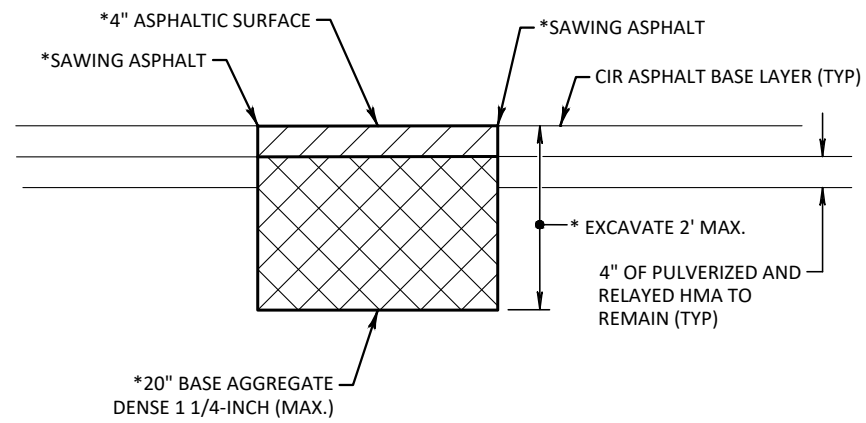
NOTE, 2' HMA APPROACH NOT SHOWN ON PLAN SHEETS

RURAL DRIVEWAY OR FIELD ENTRANCE AGGREGATE (PE, CE OR FE)

SITE NO LOCATION	PHASE 1 (2 SEC)	PHASE 2 (2 SEC)
SITE 2 STA 879+00	CTH G CLOSED	STARTING XX/XX/XX

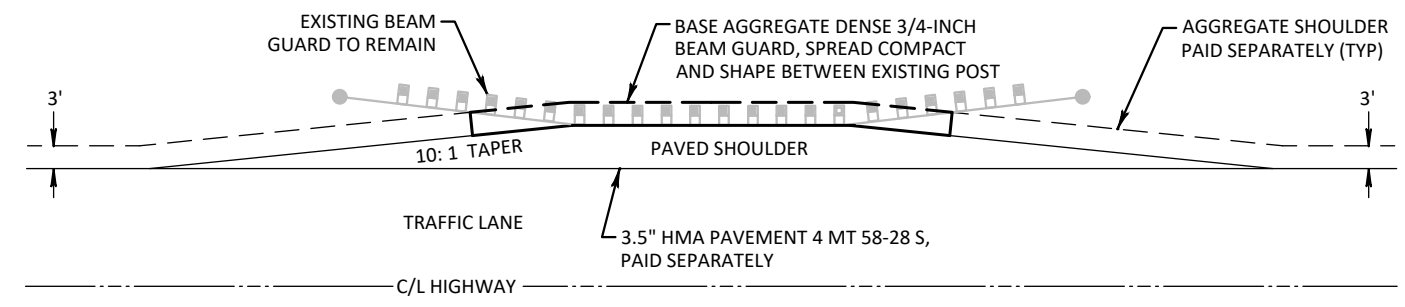
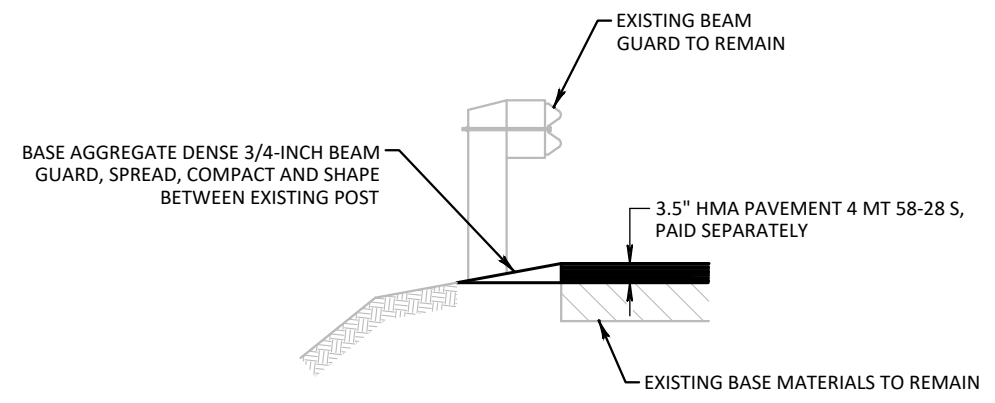
NOTE: FOLLOW STANDARD DETAIL DRAWINGS FOR ROAD CLOSURES.

PCMS MESSAGE



\* = INCIDENTAL TO BID ITEM  
"BASE REPAIR FOR CIR LAYER"

BASE REPAIR FOR CIR LAYER



BASE AGGREGATE DENSE 3/4-INCH BEAM GUARD

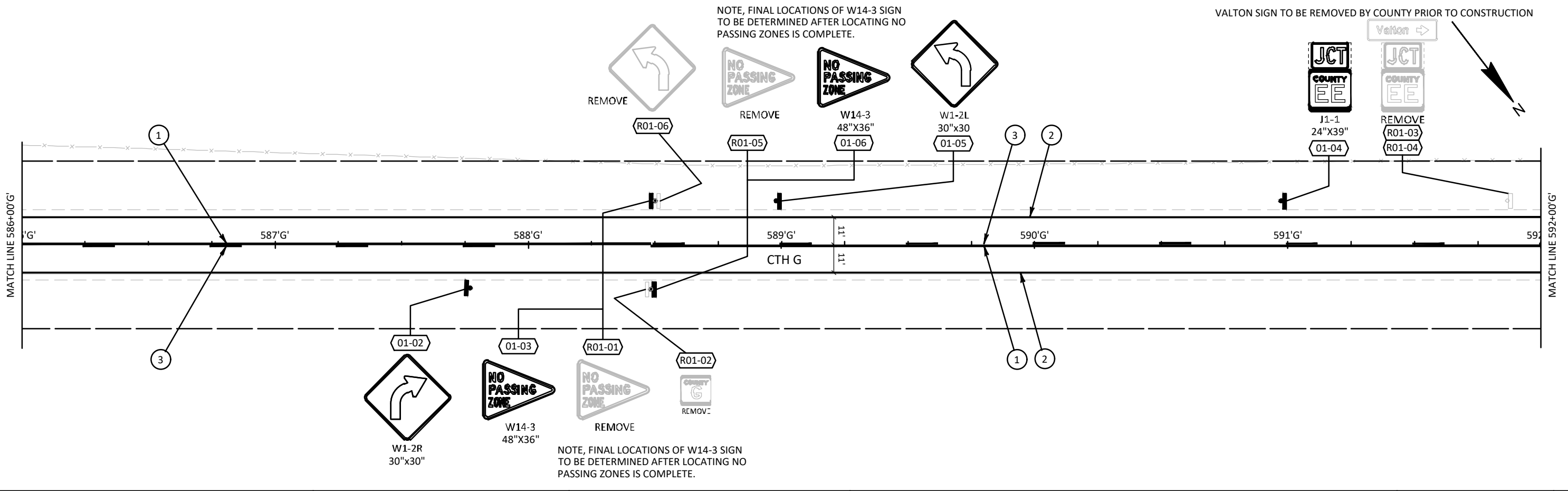
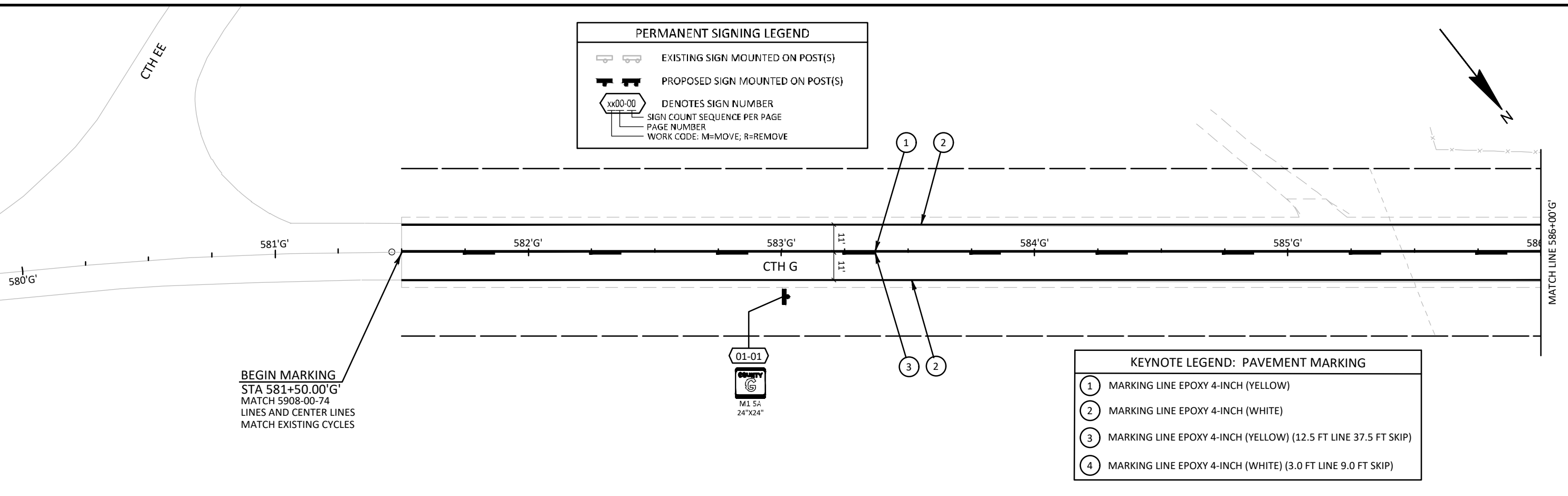
**PERMANENT SIGNING LEGEND**

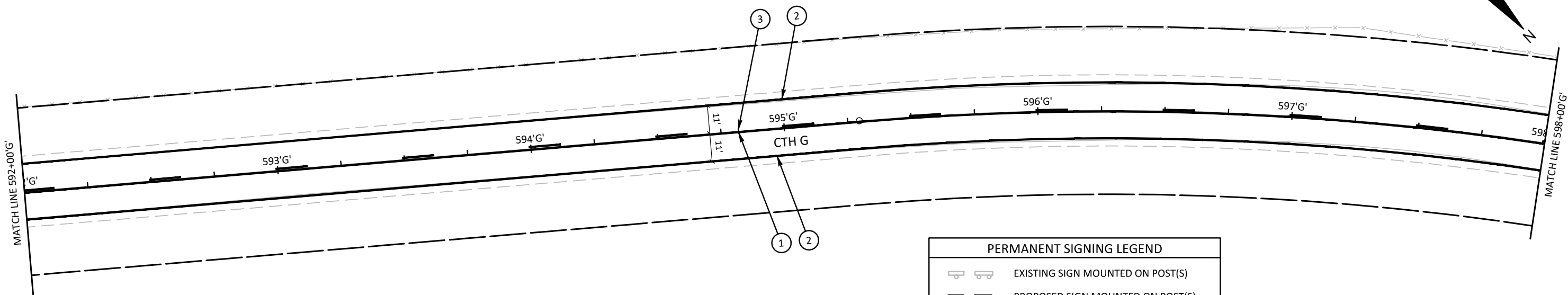
- EXISTING SIGN MOUNTED ON POST(S)
- PROPOSED SIGN MOUNTED ON POST(S)
- DENOTES SIGN NUMBER
- SIGN COUNT SEQUENCE PER PAGE
- PAGE NUMBER
- WORK CODE: M=MOVE, R=REMOVE

**KEYNOTE LEGEND: PAVEMENT MARKING**

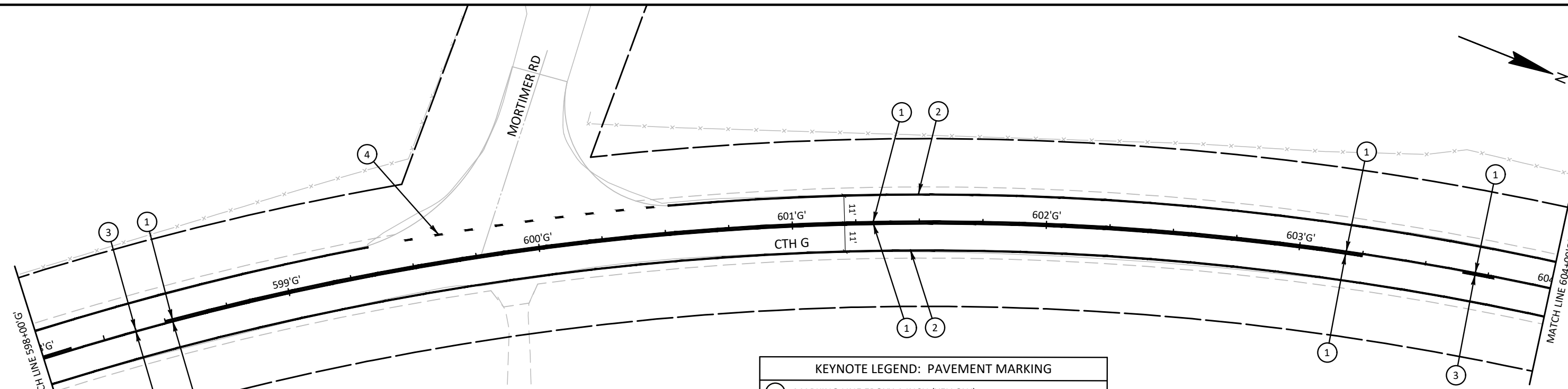
- 1 MARKING LINE EPOXY 4-INCH (YELLOW)
- 2 MARKING LINE EPOXY 4-INCH (WHITE)
- 3 MARKING LINE EPOXY 4-INCH (YELLOW) (12.5 FT LINE 37.5 FT SKIP)
- 4 MARKING LINE EPOXY 4-INCH (WHITE) (3.0 FT LINE 9.0 FT SKIP)

BEGIN MARKING  
STA 581+50.00'G'  
MATCH 5908-00-74  
LINES AND CENTER LINES  
MATCH EXISTING CYCLES





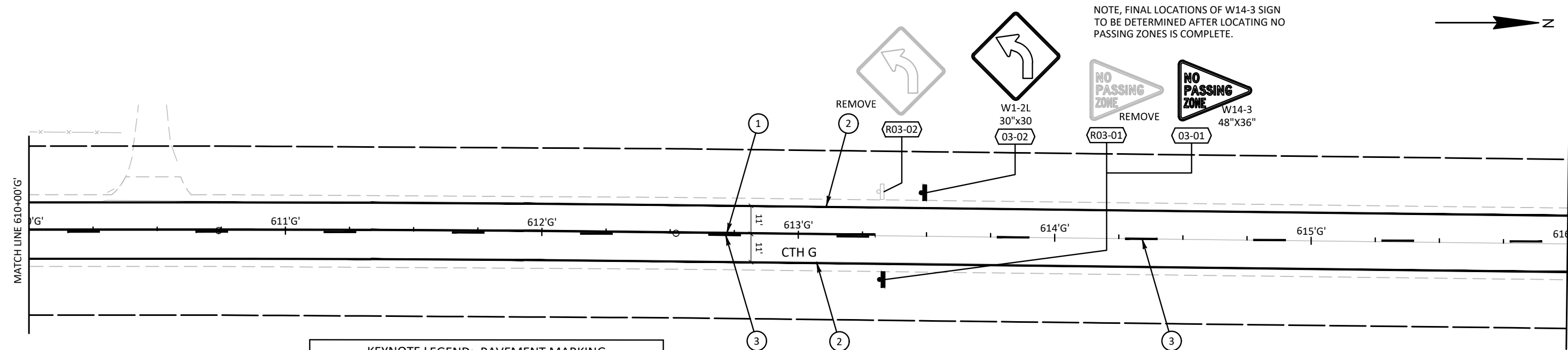
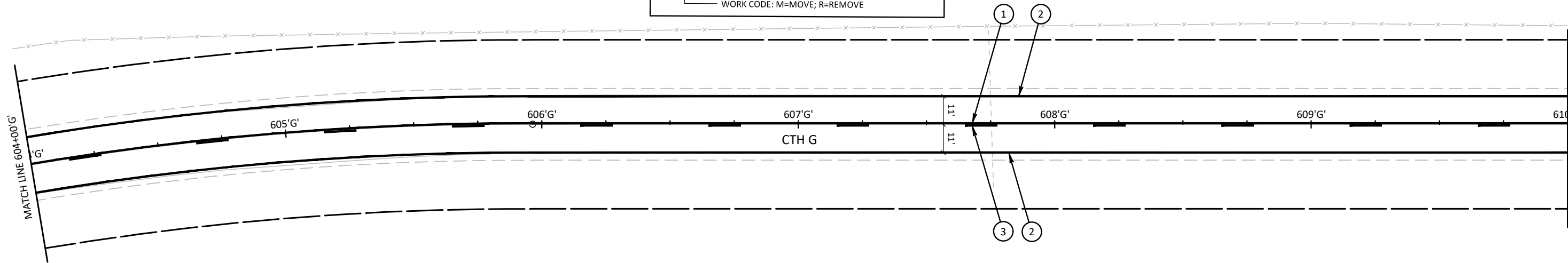
PERMANENT SIGNING LEGEND	
	EXISTING SIGN MOUNTED ON POST(S)
	PROPOSED SIGN MOUNTED ON POST(S)
	DENOTES SIGN NUMBER
	SIGN COUNT SEQUENCE PER PAGE
	PAGE NUMBER
	WORK CODE: M=MOVE; R=REMOVE



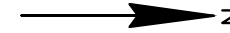
KEYNOTE LEGEND: PAVEMENT MARKING	
①	MARKING LINE EPOXY 4-INCH (YELLOW)
②	MARKING LINE EPOXY 4-INCH (WHITE)
③	MARKING LINE EPOXY 4-INCH (YELLOW) (12.5 FT LINE 37.5 FT SKIP)
④	MARKING LINE EPOXY 4-INCH (WHITE) (3.0 FT LINE 9.0 FT SKIP)

**PERMANENT SIGNING LEGEND**

- EXISTING SIGN MOUNTED ON POST(S)
- PROPOSED SIGN MOUNTED ON POST(S)
- DENOTES SIGN NUMBER
- SIGN COUNT SEQUENCE PER PAGE
- PAGE NUMBER
- WORK CODE: M=MOVE; R=REMOVE





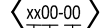



NOTE, FINAL LOCATIONS OF W14-3 SIGN TO BE DETERMINED AFTER LOCATING NO PASSING ZONES IS COMPLETE.

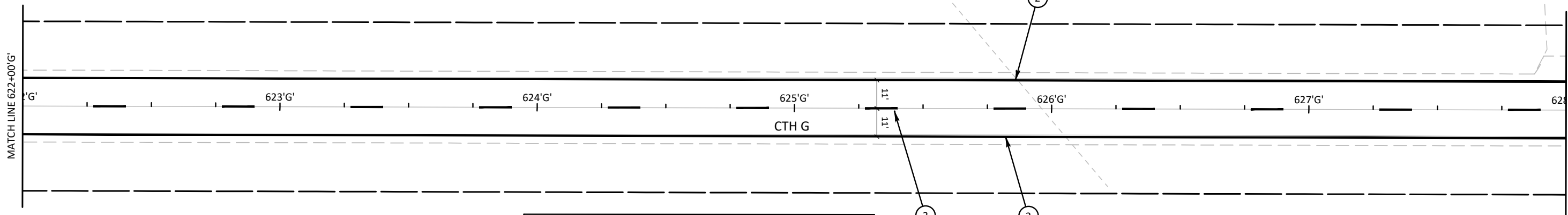
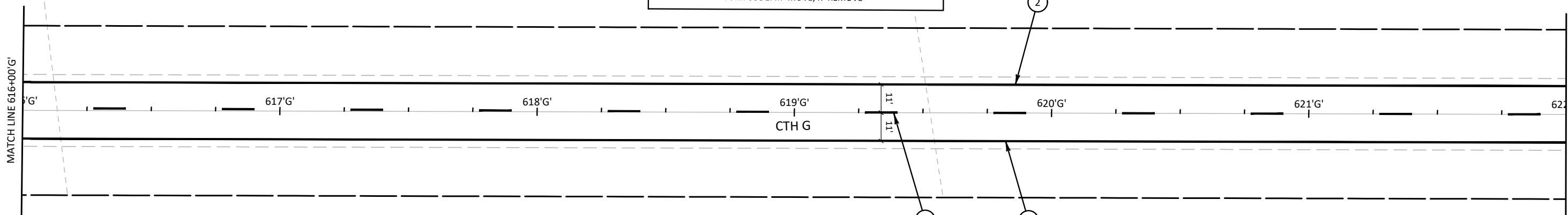
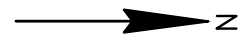


**KEYNOTE LEGEND: PAVEMENT MARKING**

- ① MARKING LINE EPOXY 4-INCH (YELLOW)
- ② MARKING LINE EPOXY 4-INCH (WHITE)
- ③ MARKING LINE EPOXY 4-INCH (YELLOW) (12.5 FT LINE 37.5 FT SKIP)
- ④ MARKING LINE EPOXY 4-INCH (WHITE) (3.0 FT LINE 9.0 FT SKIP)

**PERMANENT SIGNING LEGEND**

	EXISTING SIGN MOUNTED ON POST(S)
	PROPOSED SIGN MOUNTED ON POST(S)
	DENOTES SIGN NUMBER
	SIGN COUNT SEQUENCE PER PAGE
	PAGE NUMBER
	WORK CODE: M=MOVE; R=REMOVE



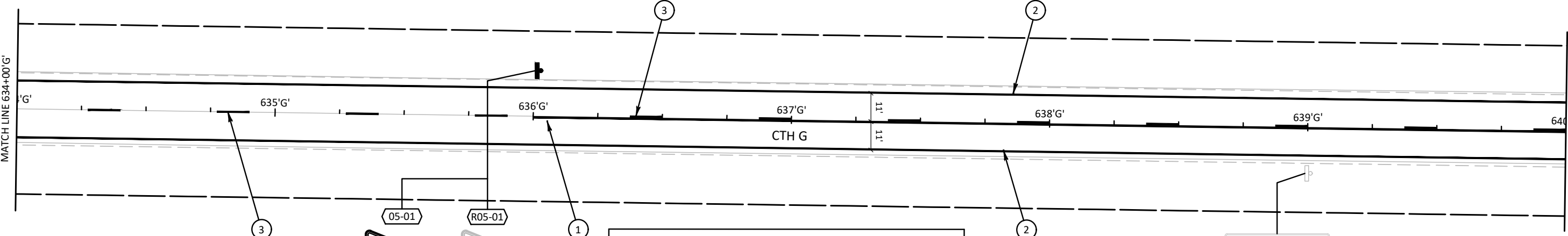
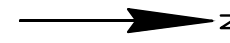
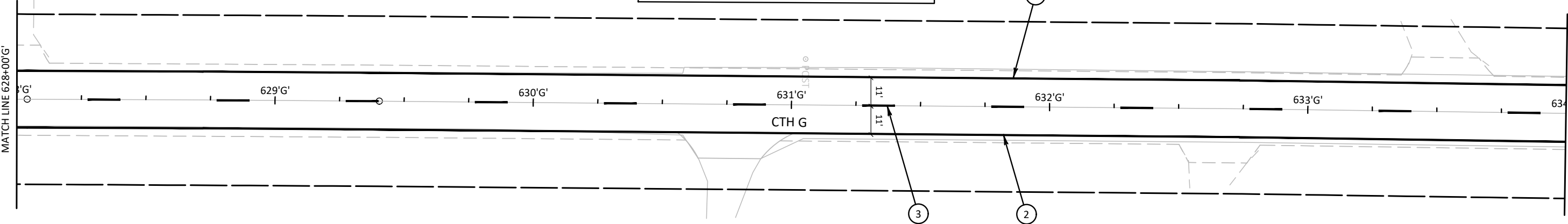
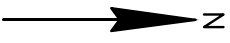
**KEYNOTE LEGEND: PAVEMENT MARKING**

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W14-3  
48"X36"

NOTE, FINAL LOCATIONS OF W14-3 SIGN TO BE DETERMINED AFTER LOCATING NO PASSING ZONES IS COMPLETE.

**KEYNOTE LEGEND: PAVEMENT MARKING**

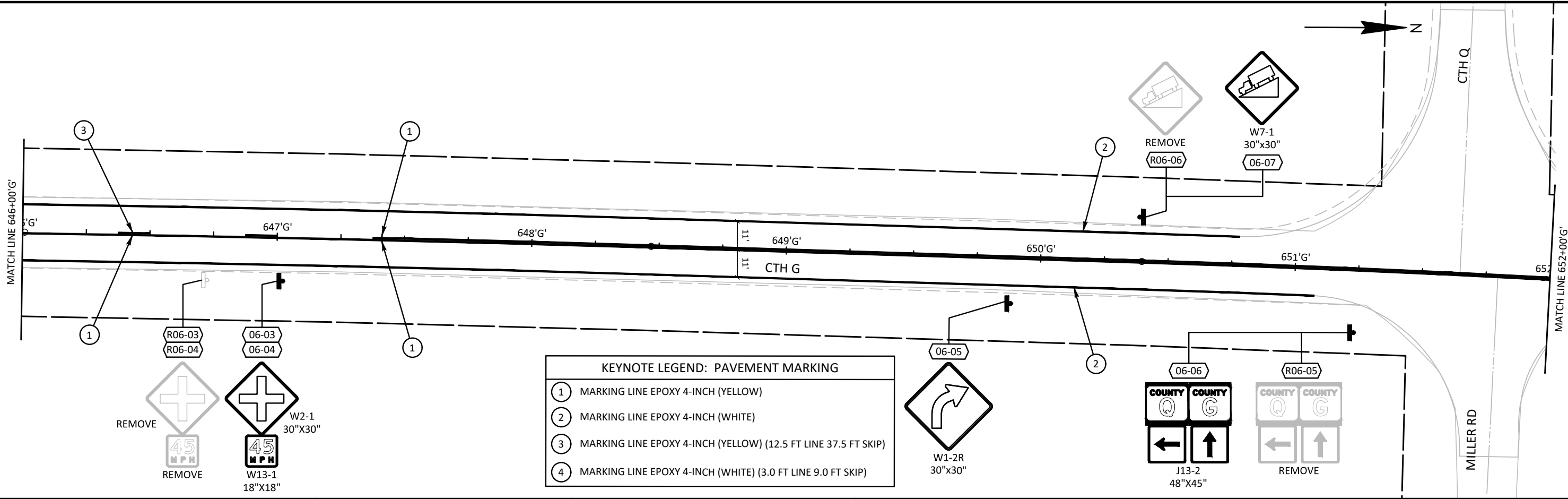
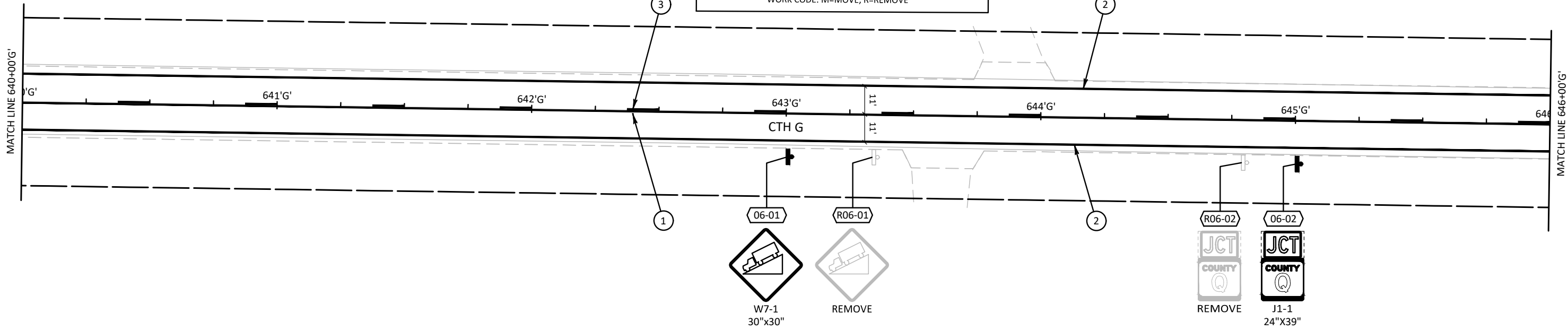
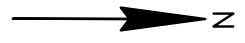
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←	St Hwy 80	6
←	Yuba	11
←	Hillsboro	14

SIGN TO BE REMOVED BY COUNTY PRIOR TO CONSTRUCTION

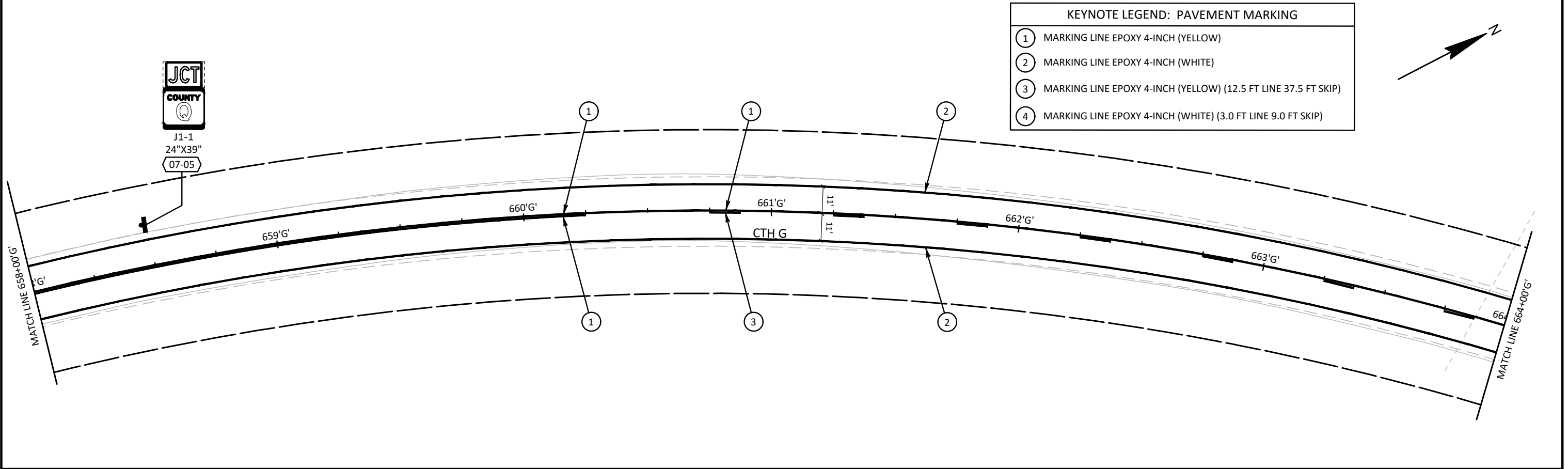
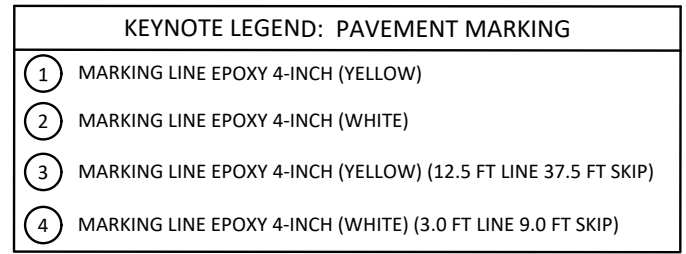
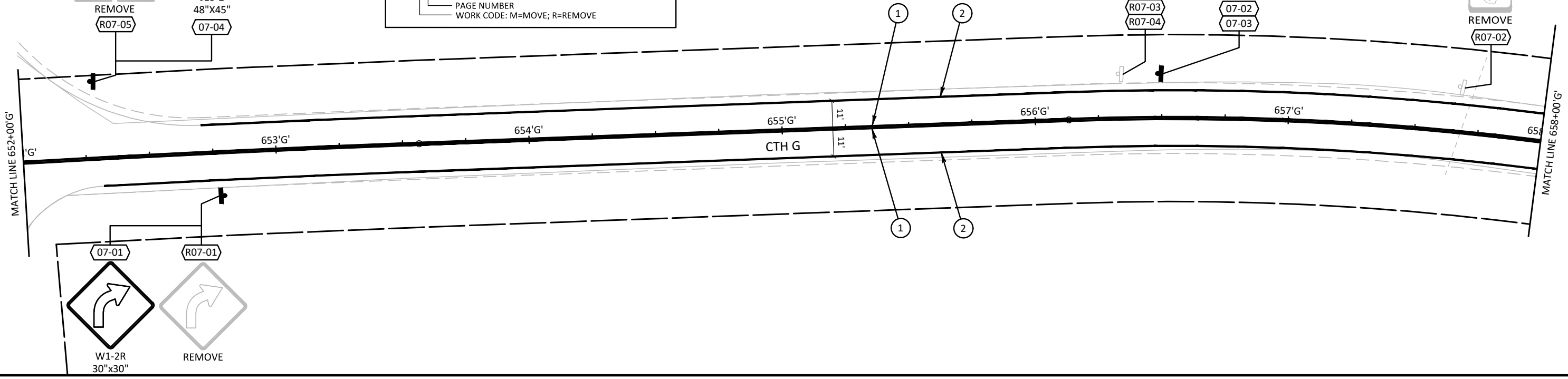
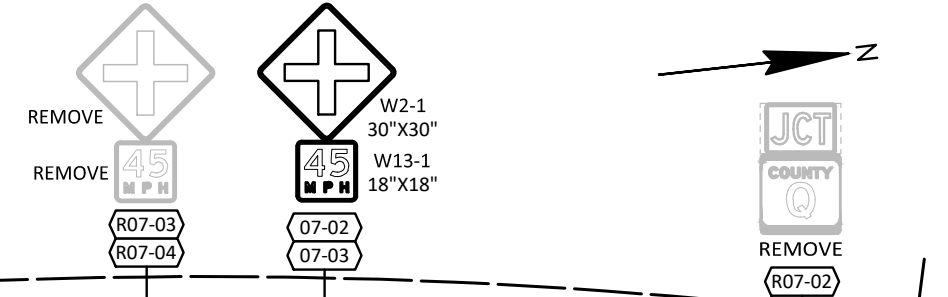
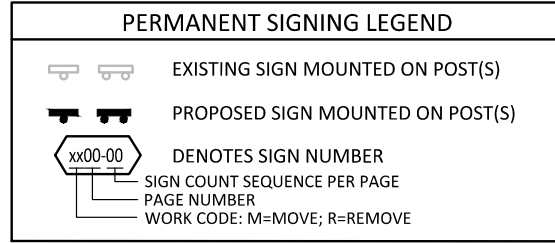
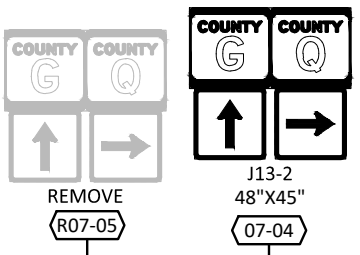
**PERMANENT SIGNING LEGEND**

- EXISTING SIGN MOUNTED ON POST(S)
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- PAGE NUMBER
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**KEYNOTE LEGEND: PAVEMENT MARKING**

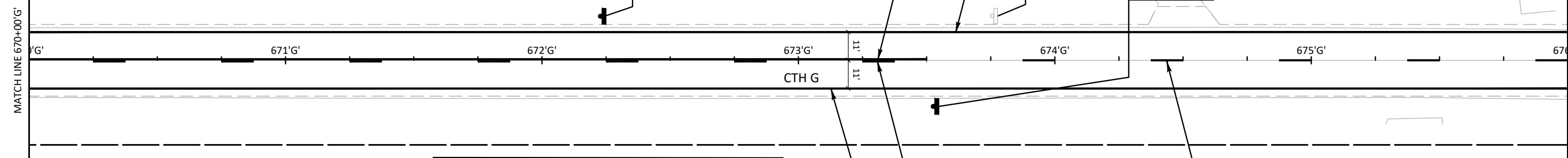
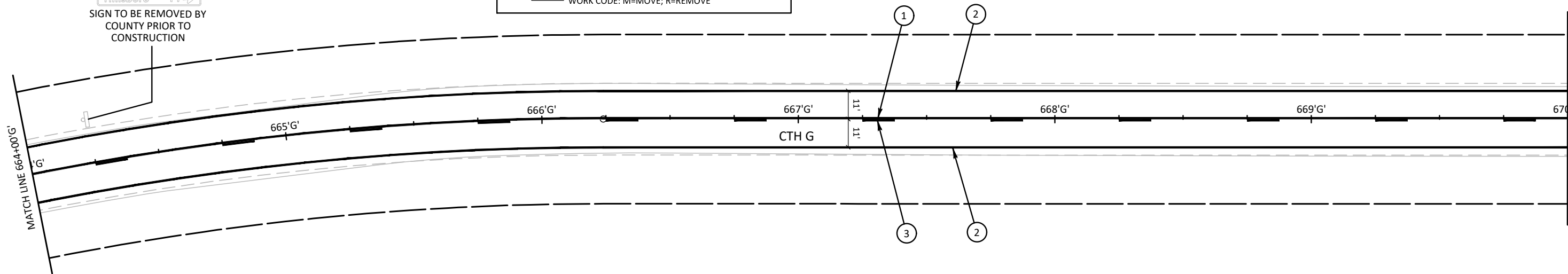
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St Hwy 80 6  
 Yuba 11  
 Hillsboro 14







SIGN TO BE REMOVED BY COUNTY PRIOR TO CONSTRUCTION

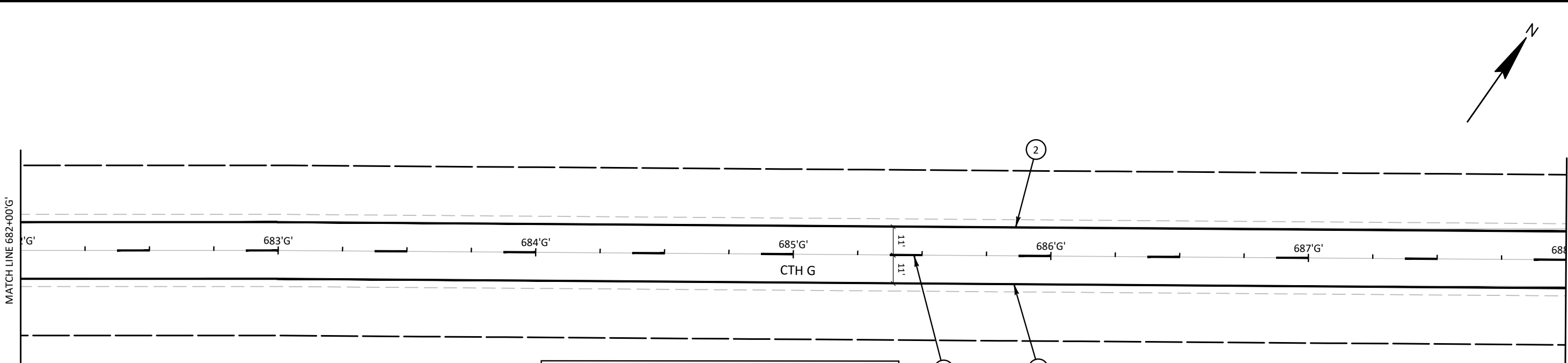
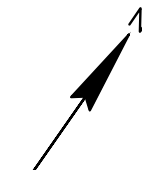
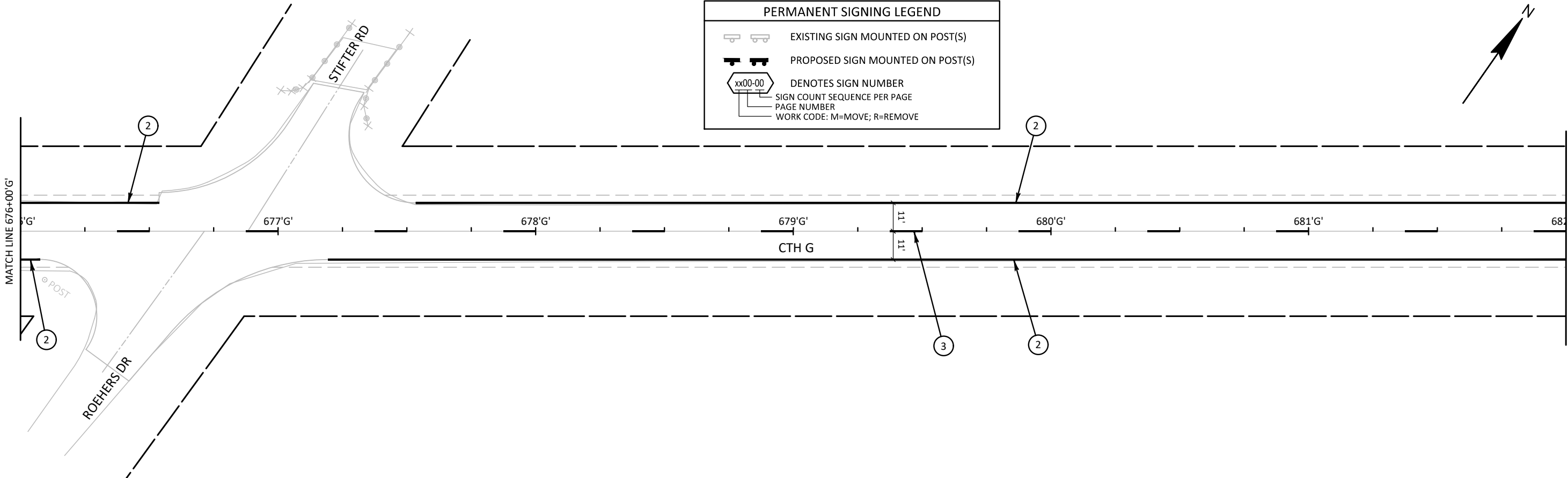


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NOTE, FINAL LOCATIONS OF W14-3 SIGN TO BE DETERMINED AFTER LOCATING NO PASSING ZONES IS COMPLETE.

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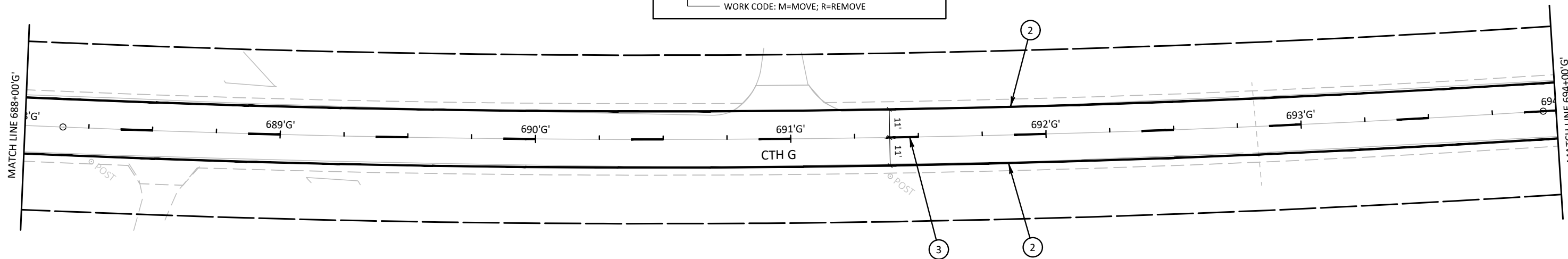
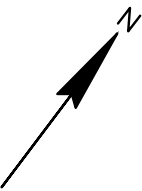


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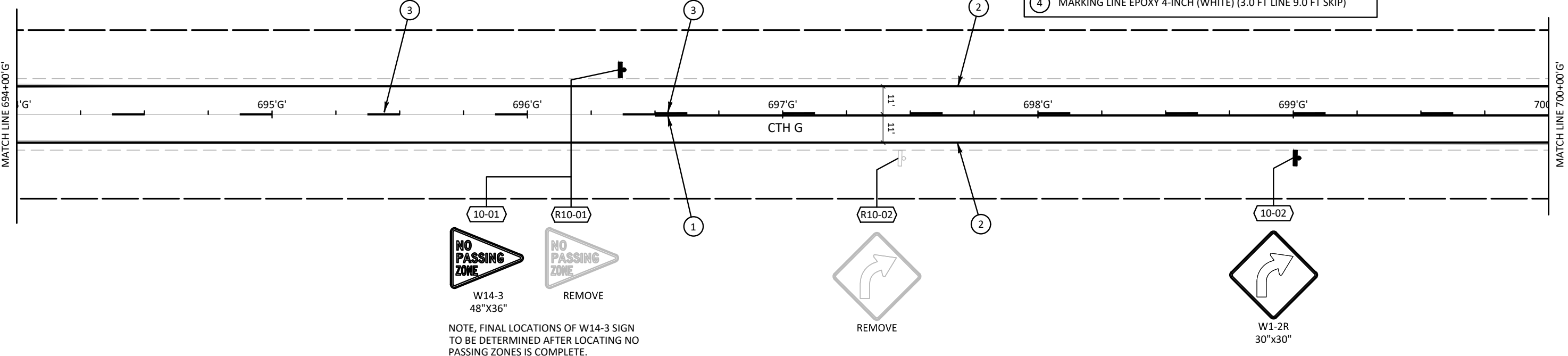
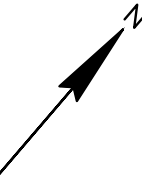
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



**KEYNOTE LEGEND: PAVEMENT MARKING**

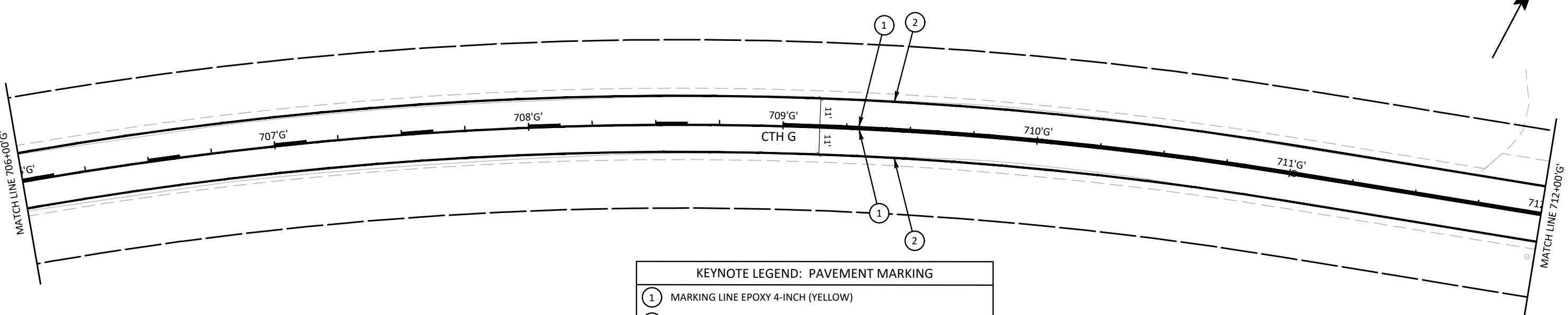
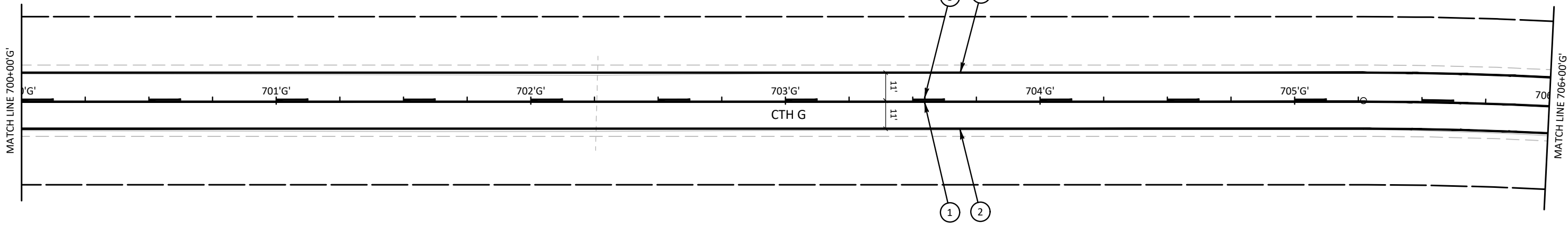
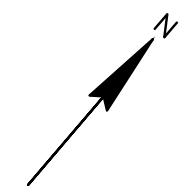
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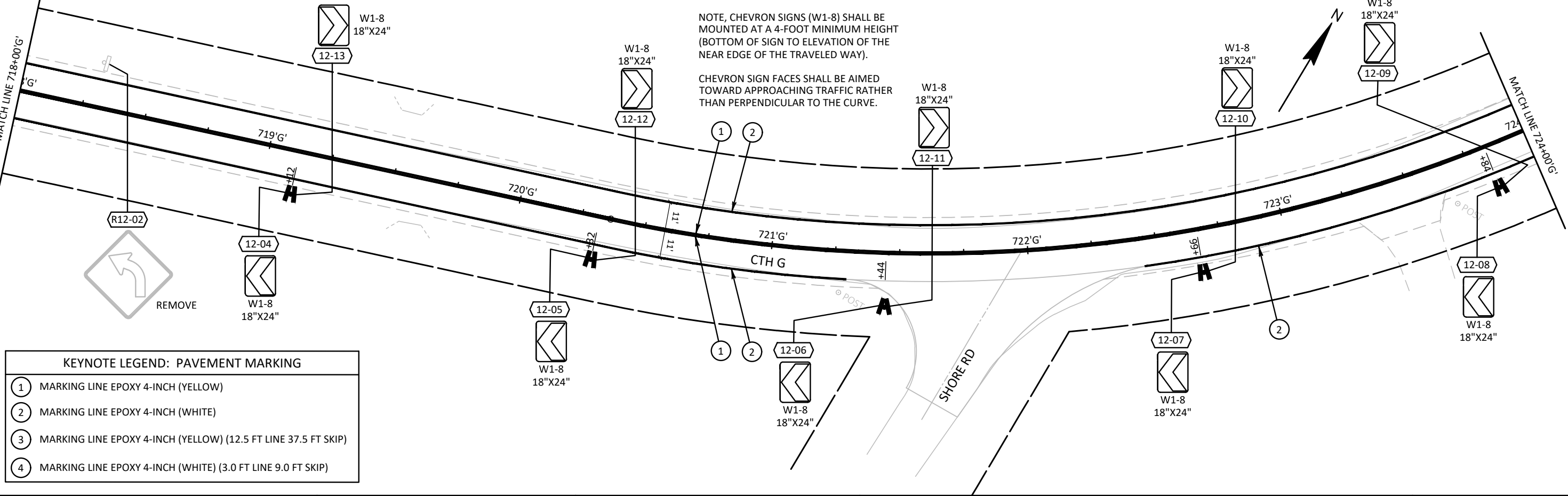
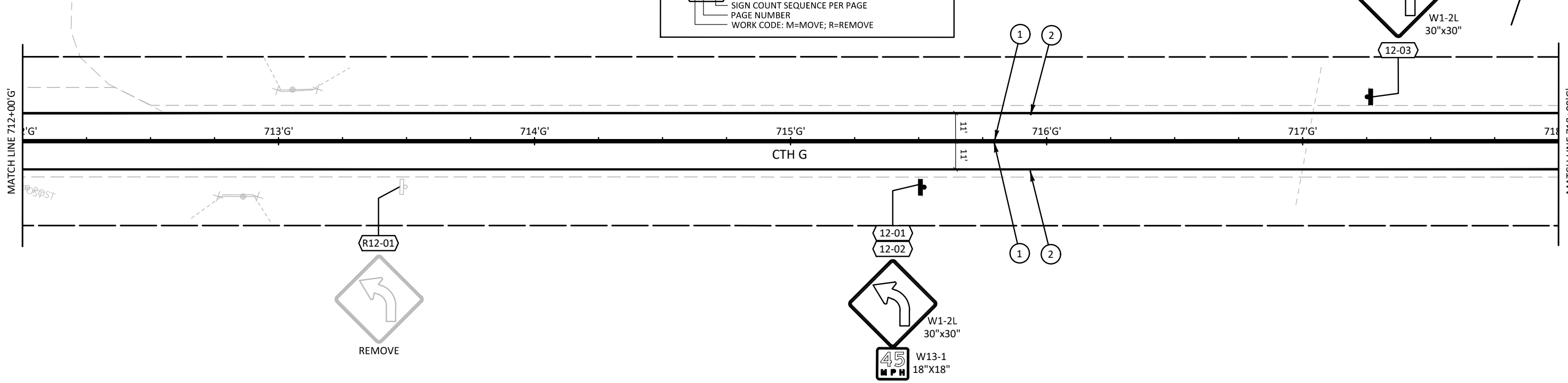


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NOTE, CHEVRON SIGNS (W1-8) SHALL BE MOUNTED AT A 4-FOOT MINIMUM HEIGHT (BOTTOM OF SIGN TO ELEVATION OF THE NEAR EDGE OF THE TRAVELED WAY).

CHEVRON SIGN FACES SHALL BE AIMED TOWARD APPROACHING TRAFFIC RATHER THAN PERPENDICULAR TO THE CURVE.

**KEYNOTE LEGEND: PAVEMENT MARKING**

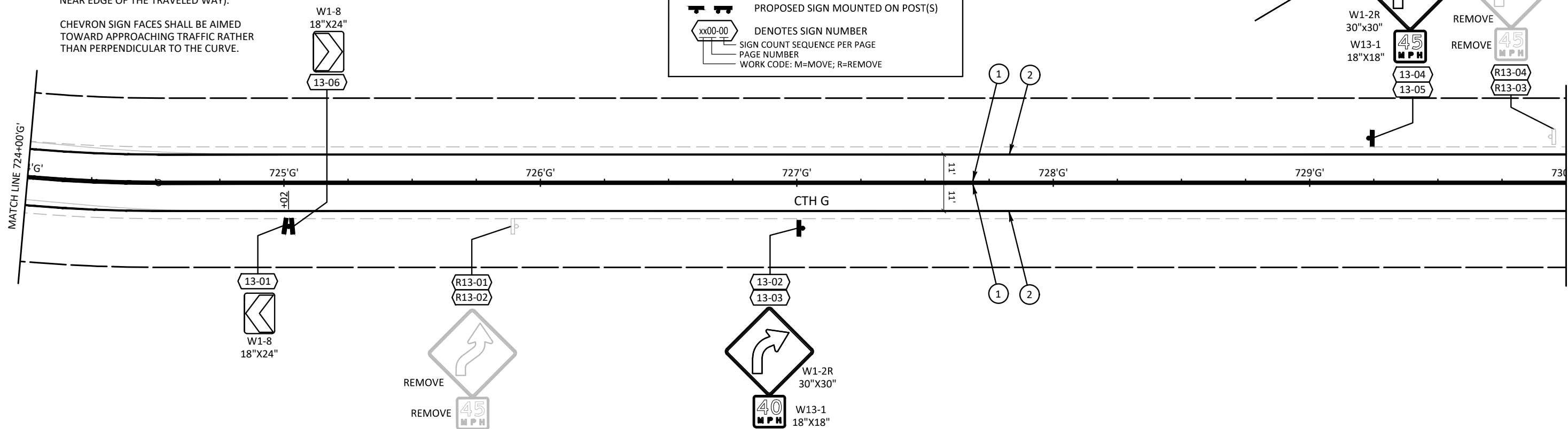
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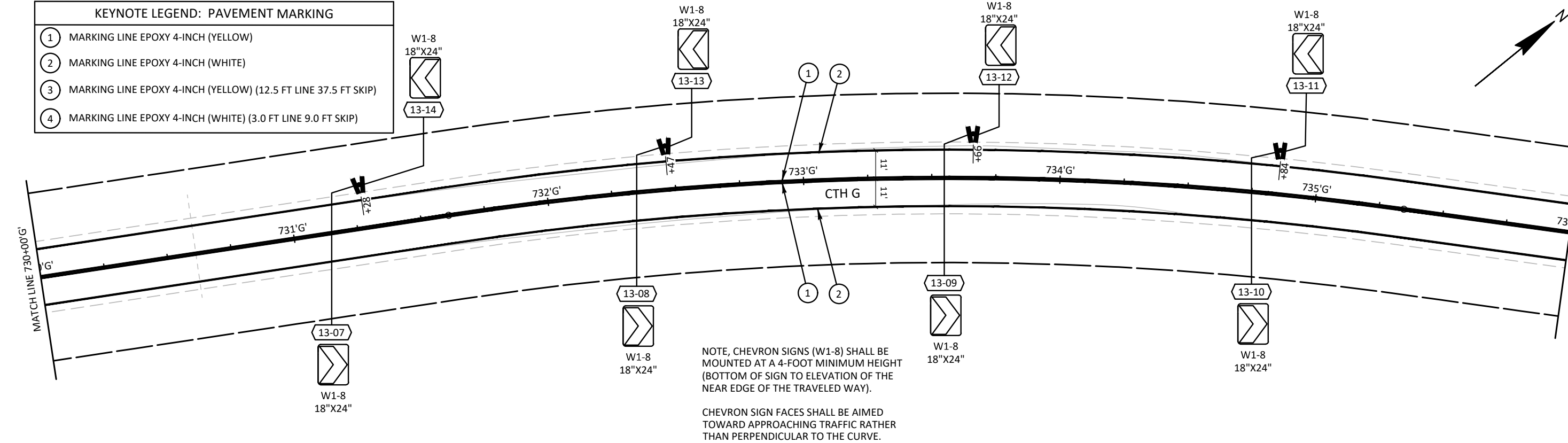
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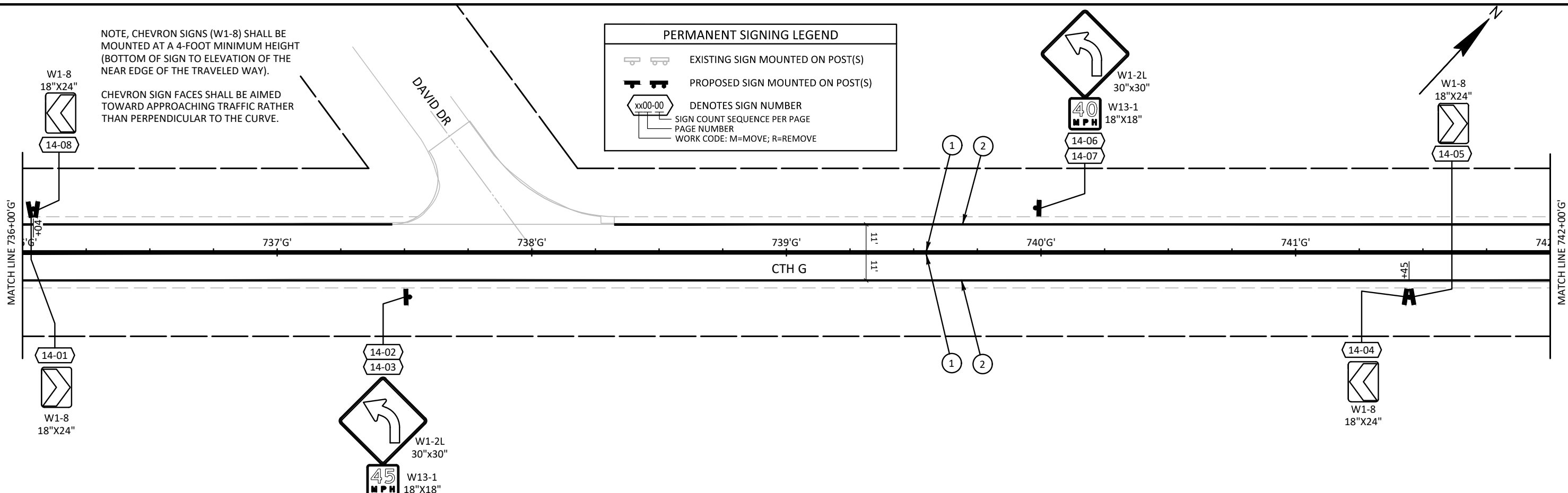
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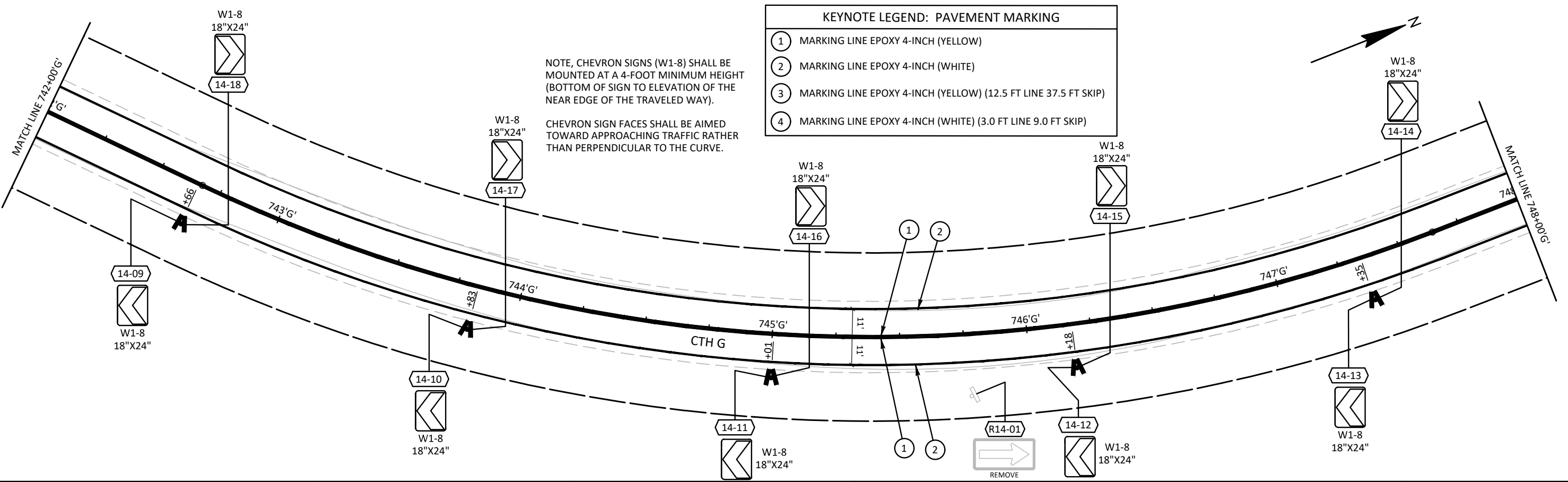
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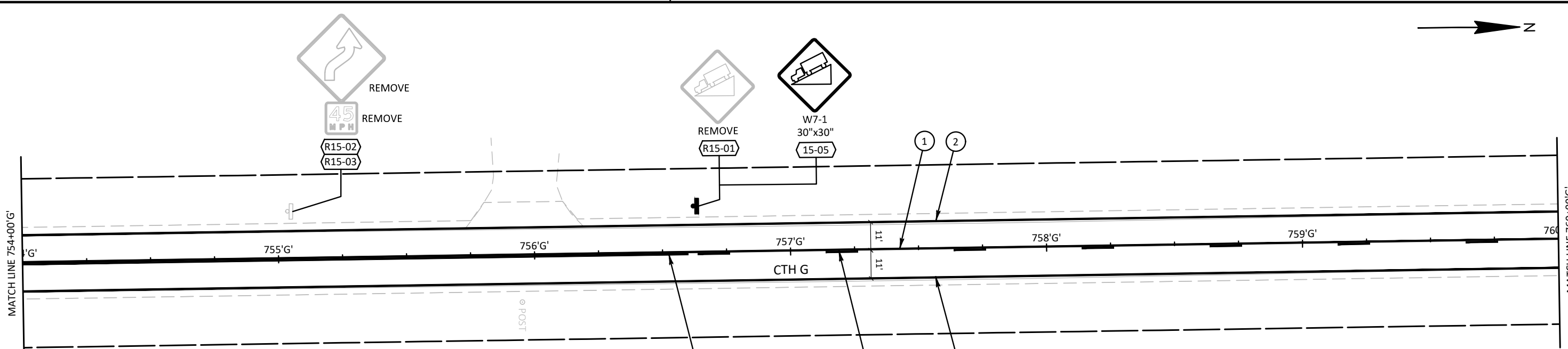
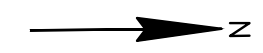
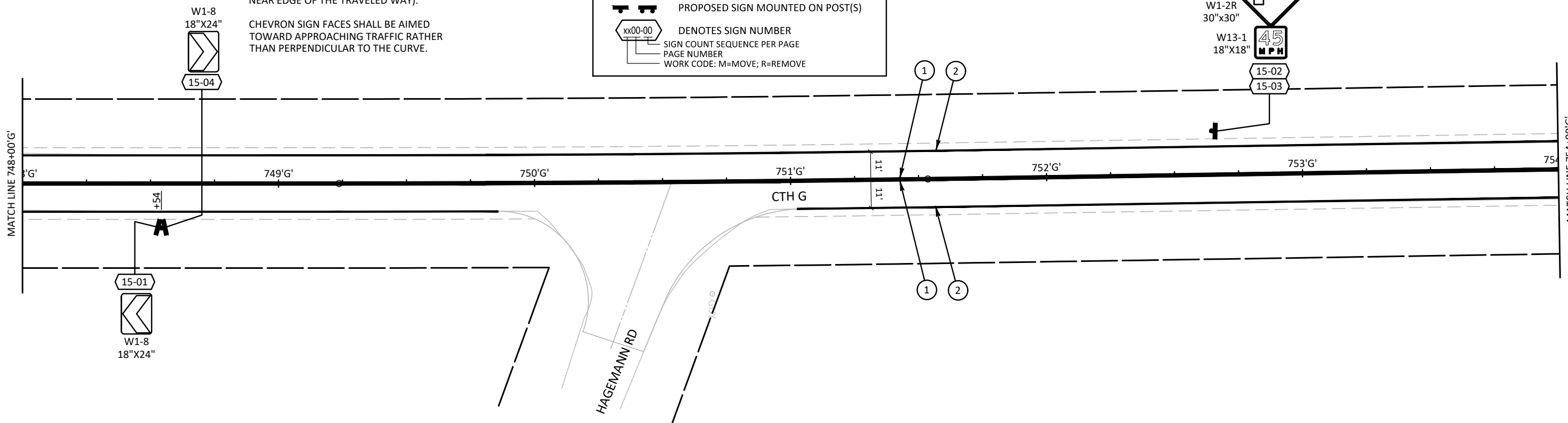
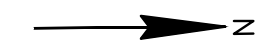
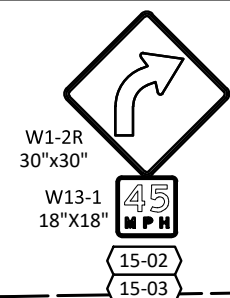
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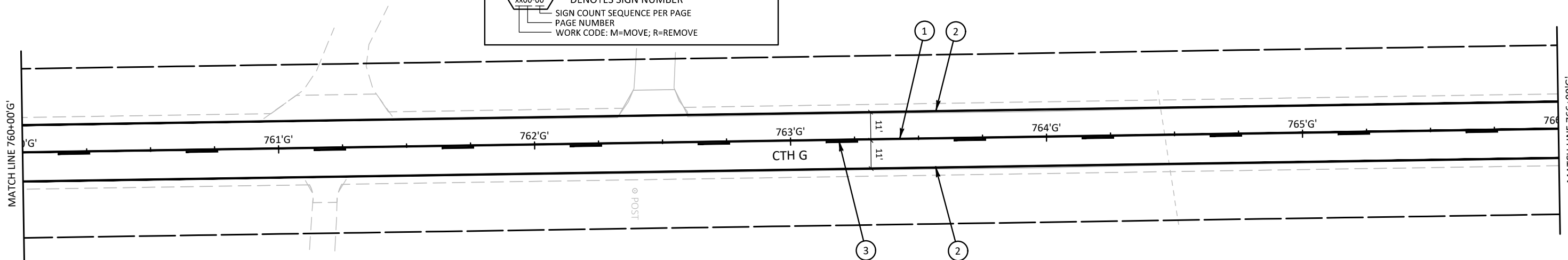
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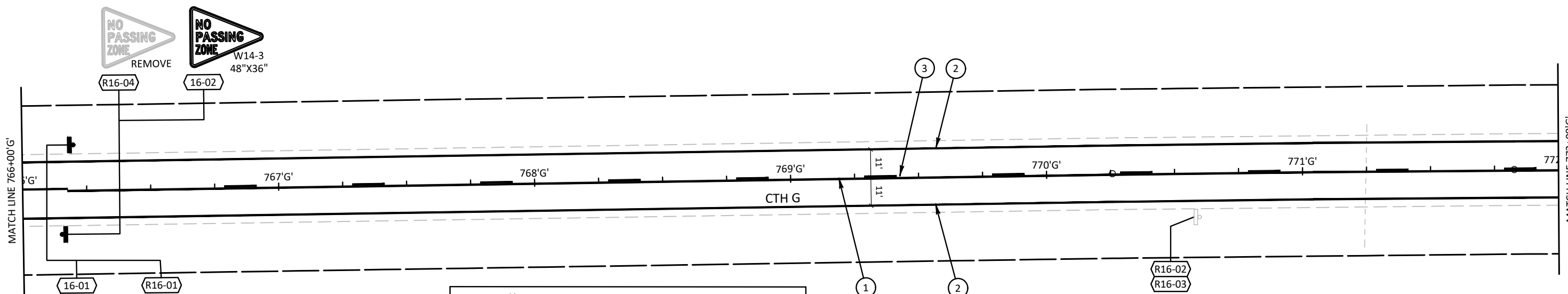
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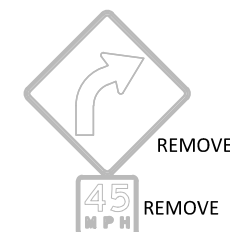








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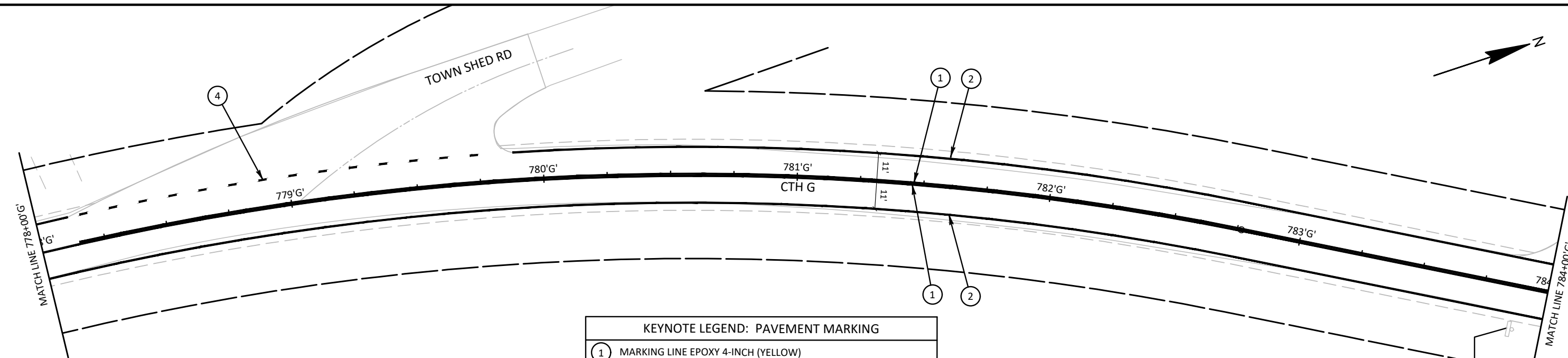
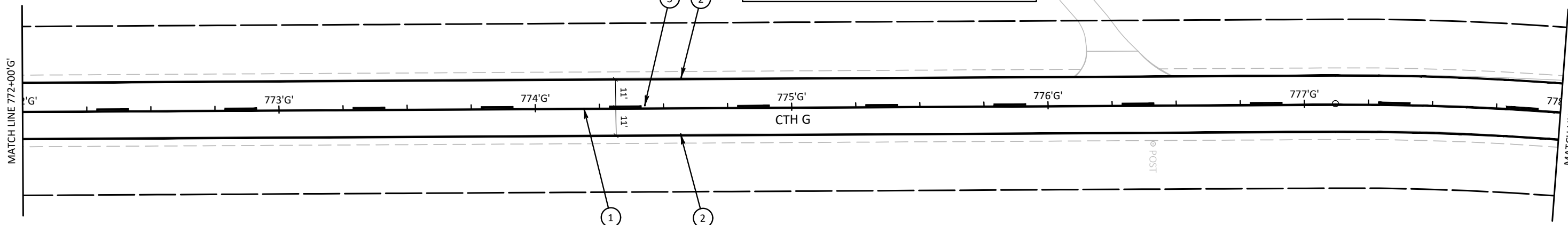
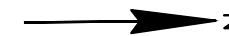
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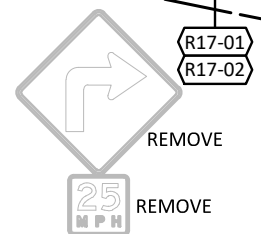
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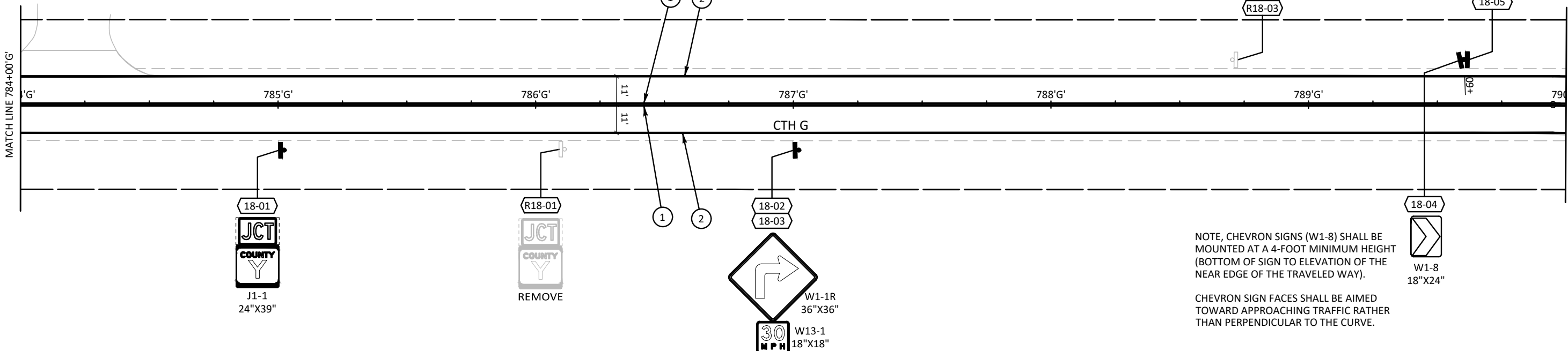


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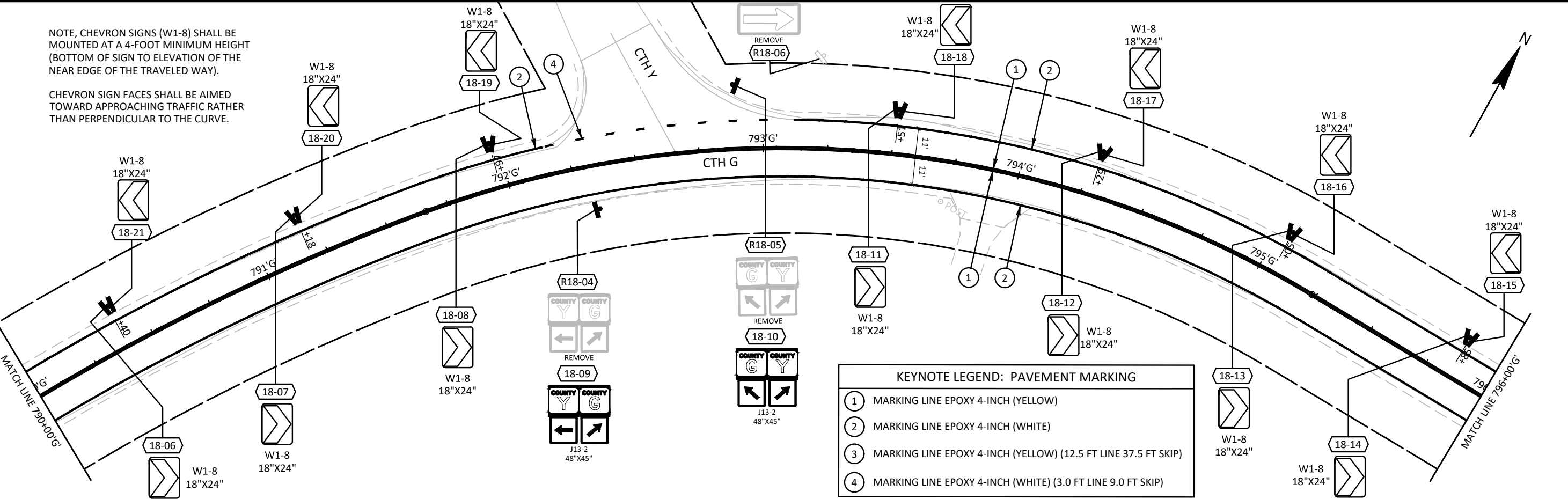


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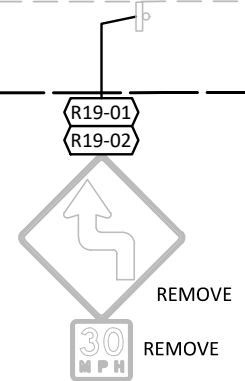
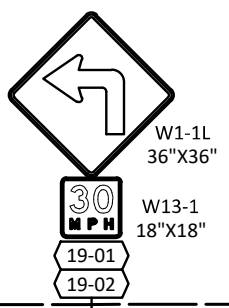
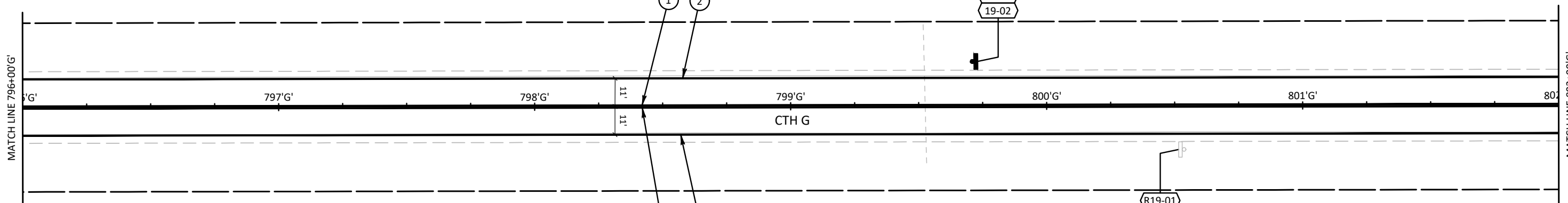


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**KEYNOTE LEGEND: PAVEMENT MARKING**

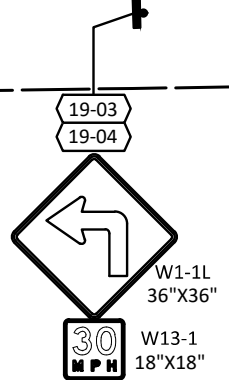
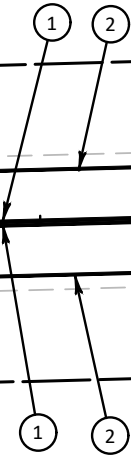
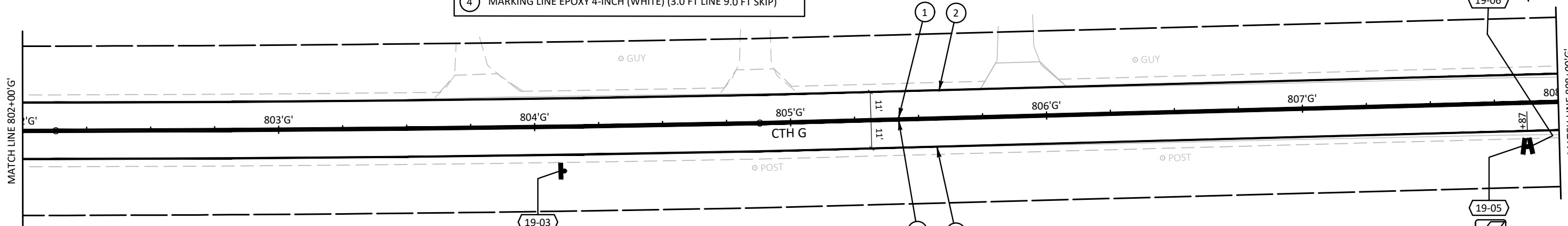
- ① MARKING LINE EPOXY 4-INCH (YELLOW)
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- ③ MARKING LINE EPOXY 4-INCH (YELLOW) (12.5 FT LINE 37.5 FT SKIP)
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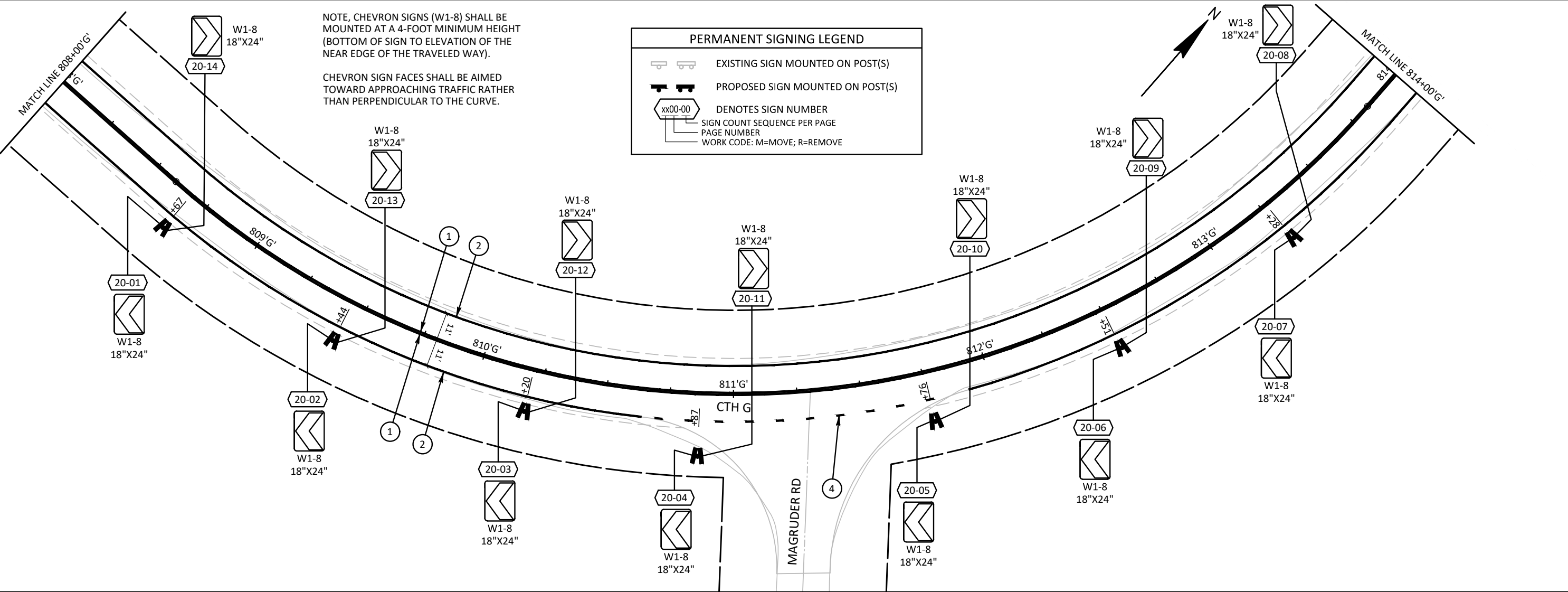


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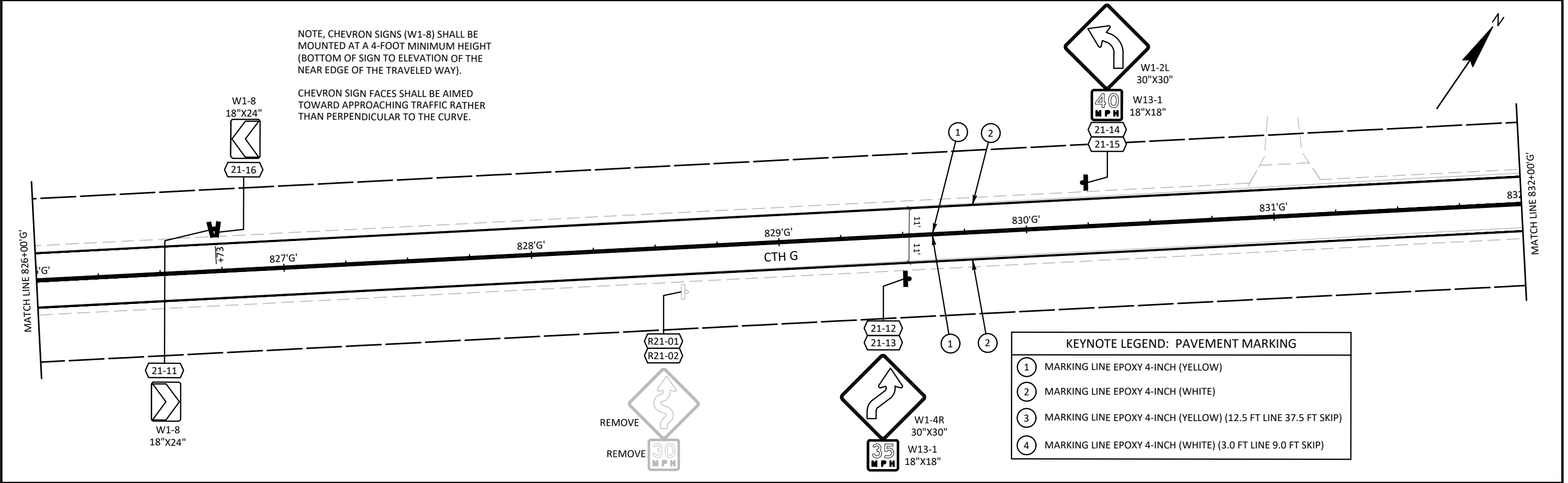
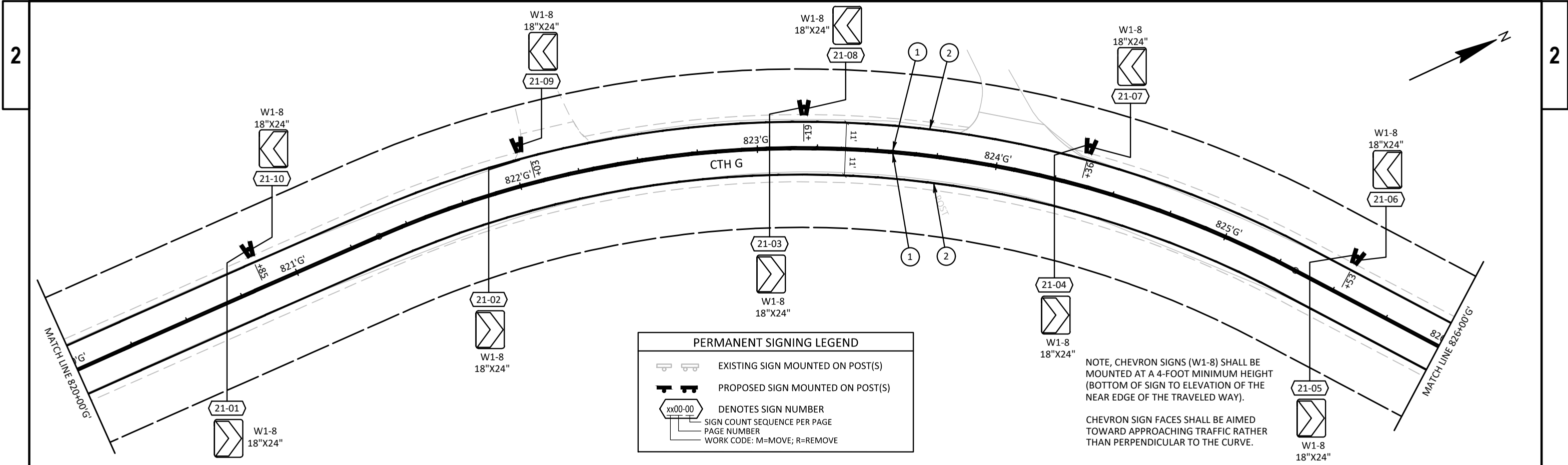
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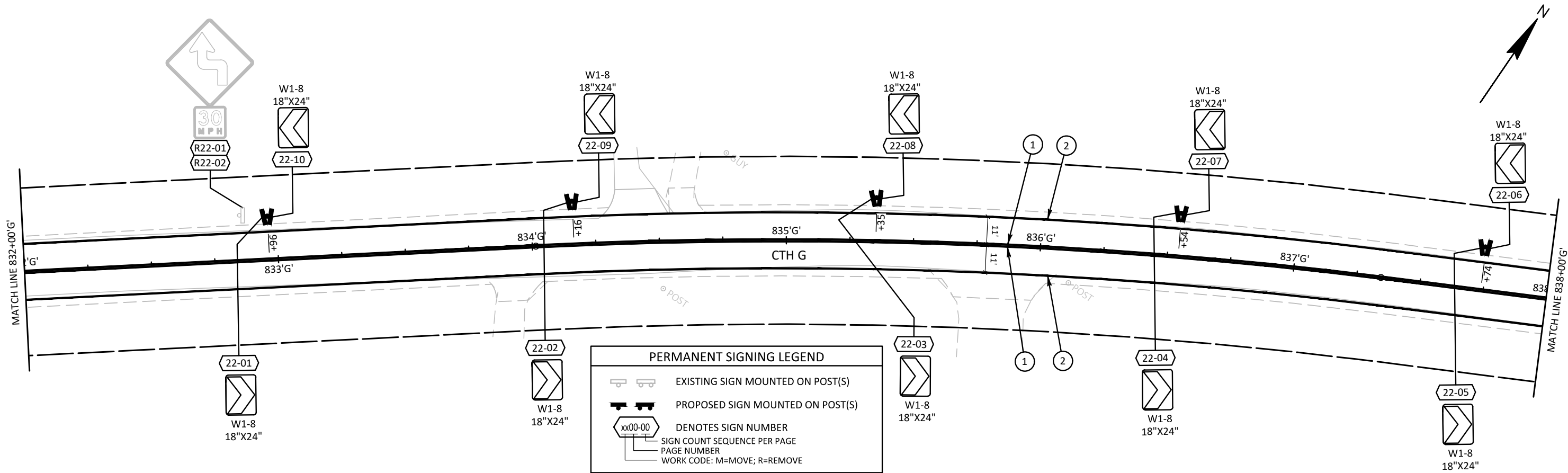
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



REMOVE  
 W5-52L 12"X36"  
 REMOVE  
 W5-52R 12"X36"  
 W13-1 18"X18"  
 W1-1R 36"X36"  
 W1-2R 30"X30"  
 W13-1 18"X18"  
 40 MPH







**PERMANENT SIGNING LEGEND**

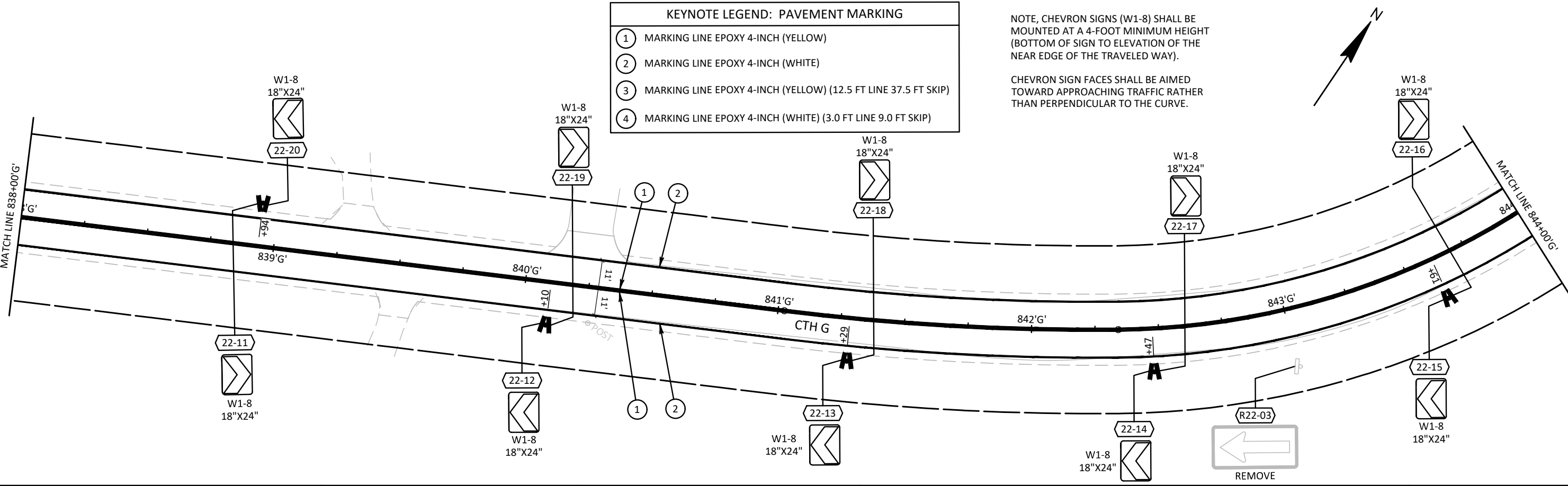
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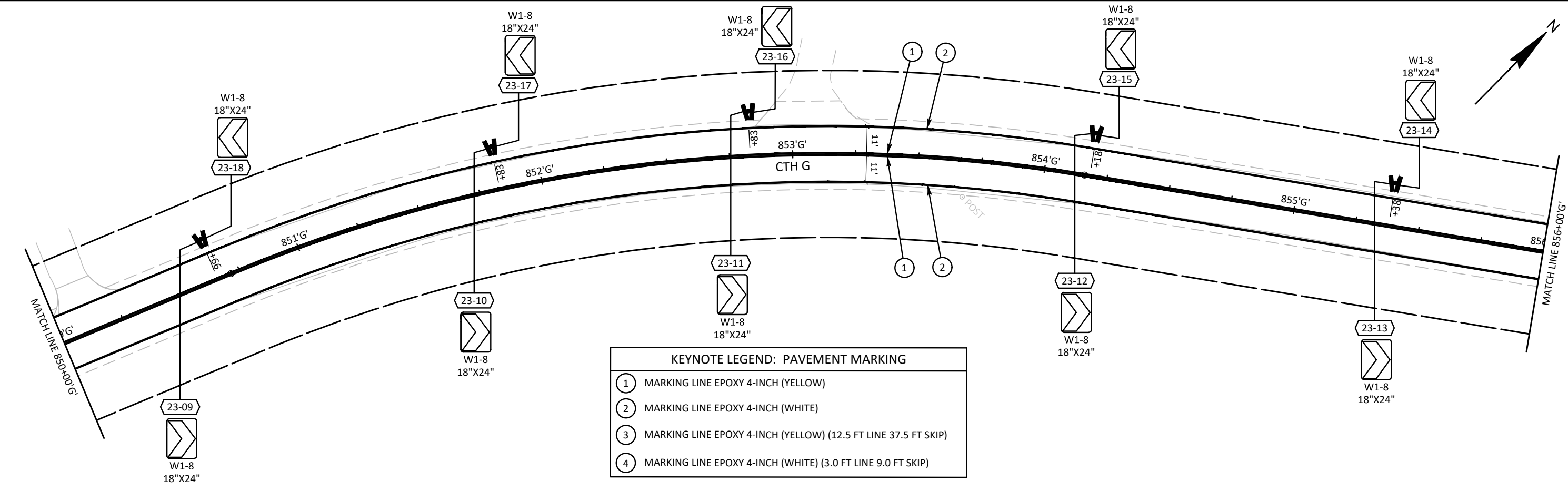
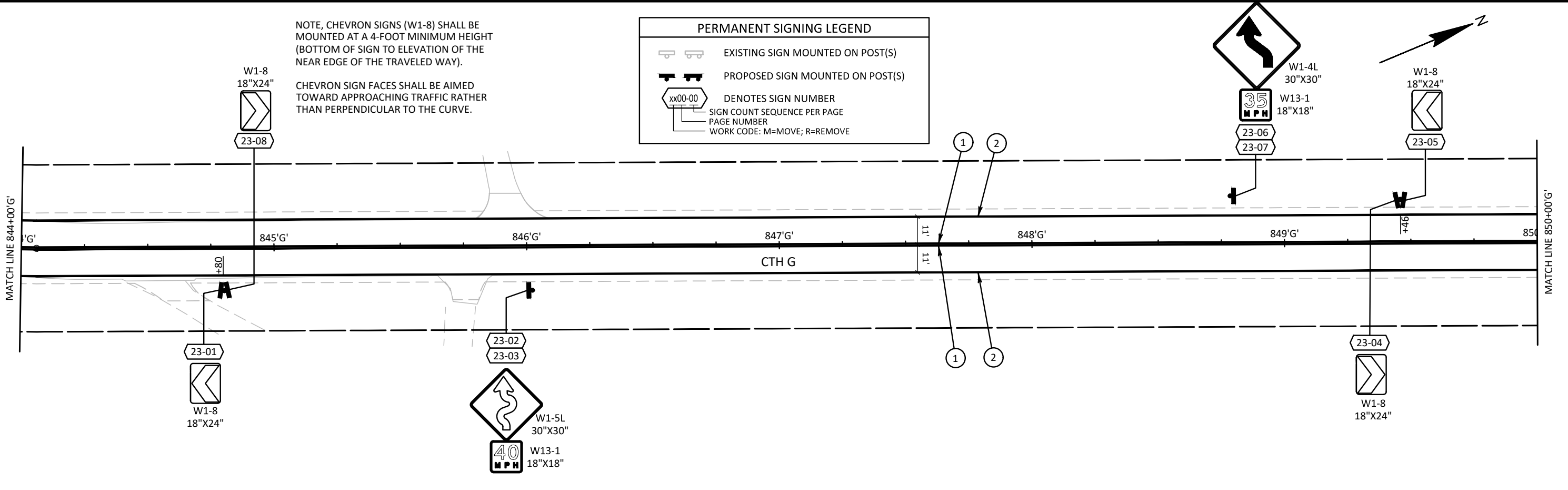
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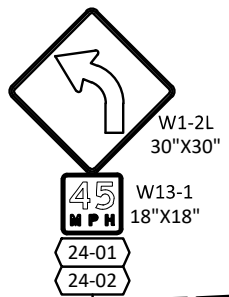
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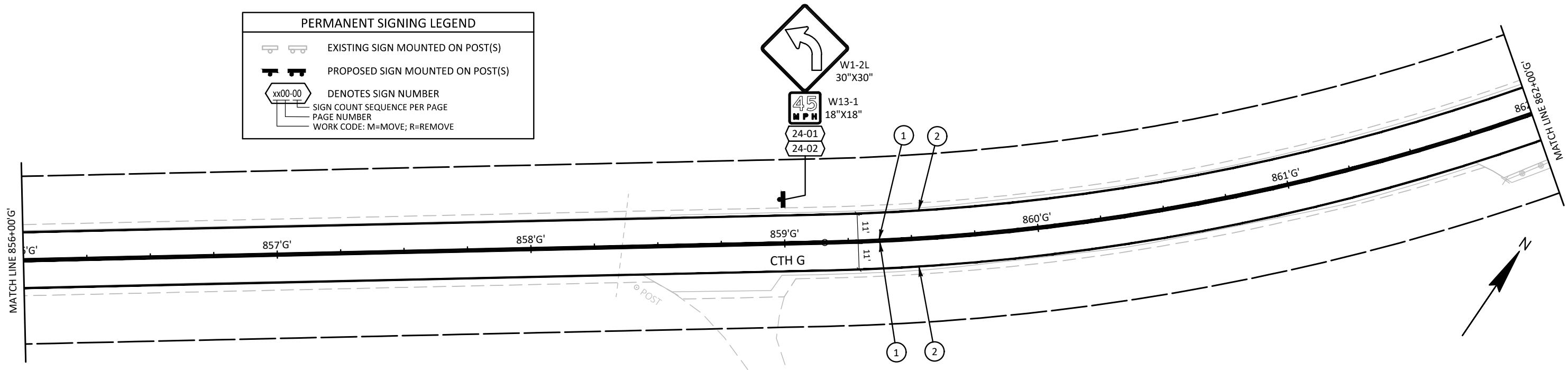
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MATCH LINE 856+00'G'

MATCH LINE 862+00'G'

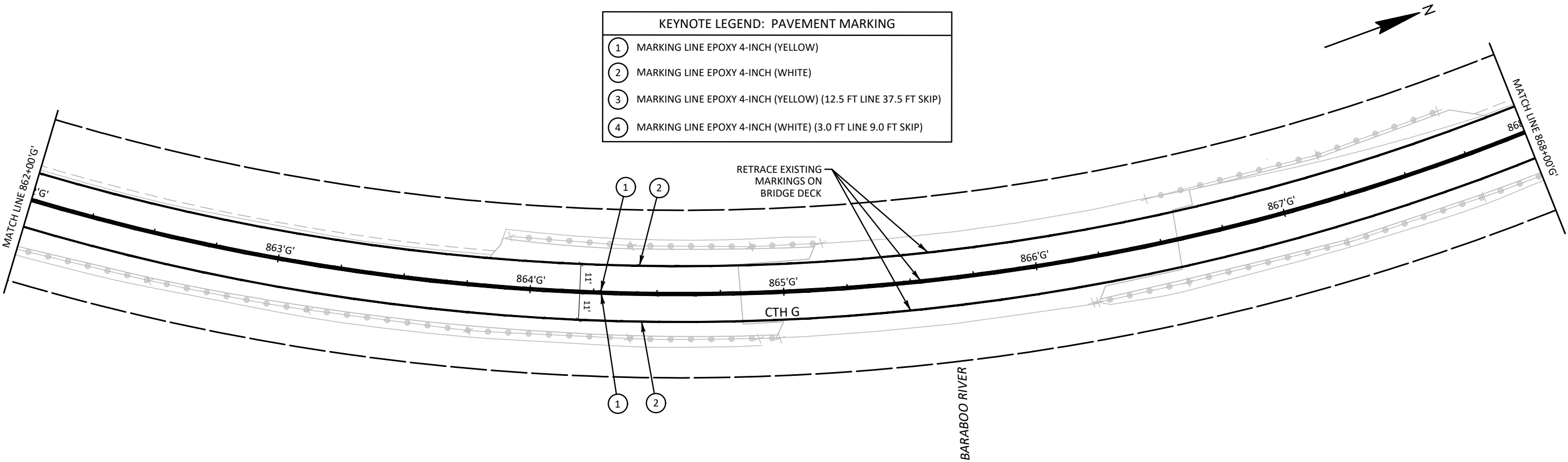


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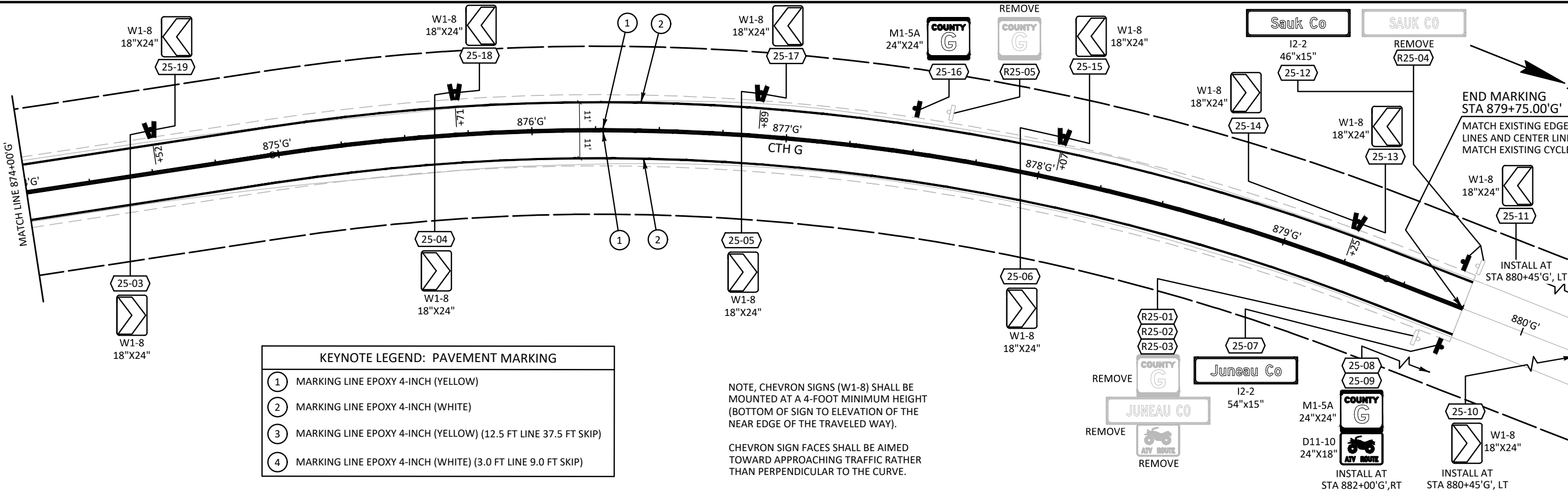
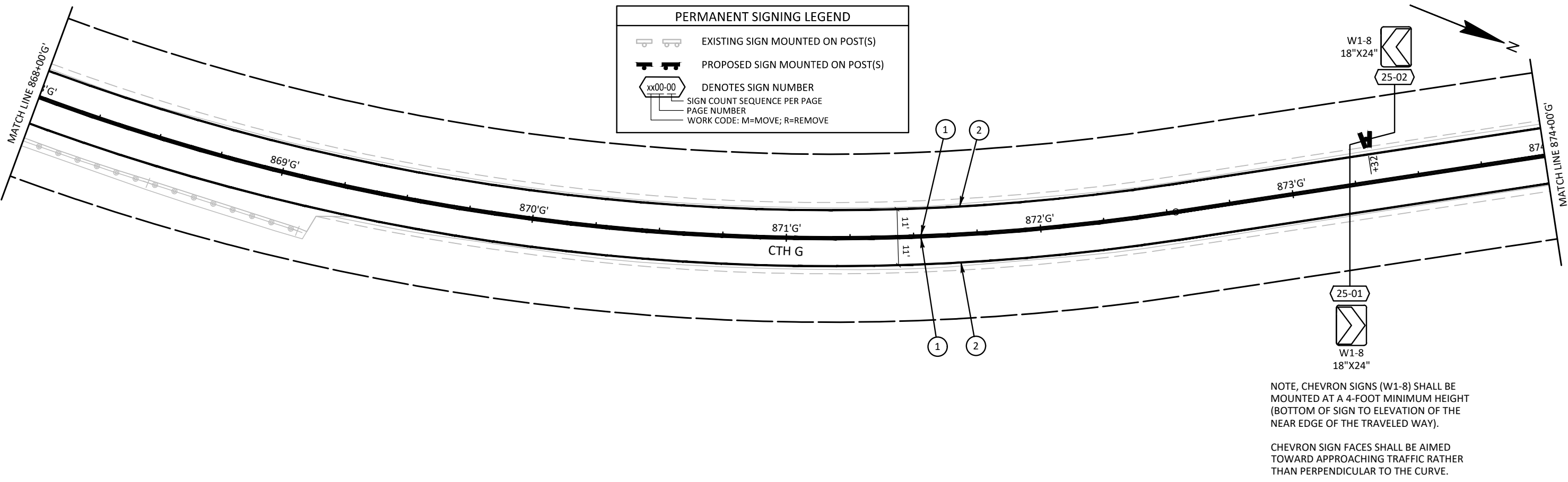
MATCH LINE 862+00'G'

MATCH LINE 868+00'G'



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## Estimate Of Quantities By Plan Sets

5908-00-75

Line	Item	Item Description	Unit	Total	Qty
0002	204.0115	Removing Asphaltic Surface Butt Joints	SY	4,734.000	4,734.000
0004	204.0120	Removing Asphaltic Surface Milling	SY	2,336.000	2,336.000
0008	211.0700.S	Prepare Foundation for CIR Base Layer (project) 02. 5908-00-75	EACH	1.000	1.000
0010	211.0800.S	Base Repair for CIR Layer	CY	465.000	465.000
0014	213.0100	Finishing Roadway (project) 02. 5908-00-75	EACH	1.000	1.000
0016	305.0110	Base Aggregate Dense 3/4-Inch	TON	4,499.000	4,499.000
0018	327.1000.S	CIR Asphaltic Base Layer	SY	69,742.000	69,742.000
0020	455.0605	Tack Coat	GAL	8,727.000	8,727.000
0022	455.0770.S	Asphalt Stabilizing Agent	TON	312.000	312.000
0028	460.2000	Incentive Density HMA Pavement	DOL	350.000	350.000
0030	460.2005	Incentive Density PWL HMA Pavement	DOL	13,660.000	13,660.000
0032	460.2007	Incentive Density HMA Pavement Longitudinal Joints	DOL	59,230.000	59,230.000
0034	460.2010	Incentive Air Voids HMA Pavement	DOL	13,960.000	13,960.000
0036	460.6223	HMA Pavement 3 MT 58-28 S	TON	537.000	537.000
0038	460.6224	HMA Pavement 4 MT 58-28 S	TON	14,647.000	14,647.000
0040	465.0120	Asphaltic Surface Driveways and Field Entrances	TON	59.000	59.000
0042	614.0400	Adjusting Steel Plate Beam Guard	LF	412.500	412.500
0046	618.0100	Maintenance And Repair of Haul Roads (project) 02. 5908-00-75	EACH	1.000	1.000
0048	619.1000	Mobilization	EACH	0.500	0.500
0050	624.0100	Water	MGAL	199.000	199.000
0052	634.0612	Posts Wood 4x6-Inch X 12-FT	EACH	4.000	4.000
0054	634.0614	Posts Wood 4x6-Inch X 14-FT	EACH	66.000	66.000
0056	634.0616	Posts Wood 4x6-Inch X 16-FT	EACH	15.000	15.000
0058	634.0618	Posts Wood 4x6-Inch X 18-FT	EACH	33.000	33.000
0060	637.2210	Signs Type II Reflective H	SF	111.400	111.400
0062	637.2230	Signs Type II Reflective F	SF	666.730	666.730
0064	638.2602	Removing Signs Type II	EACH	62.000	62.000
0066	638.3000	Removing Small Sign Supports	EACH	45.000	45.000
0068	642.5201	Field Office Type C	EACH	0.500	0.500
0070	643.0420	Traffic Control Barricades Type III	DAY	1,200.000	1,200.000
0072	643.0705	Traffic Control Warning Lights Type A	DAY	2,400.000	2,400.000
0074	643.0900	Traffic Control Signs	DAY	2,450.000	2,450.000
0076	643.0920	Traffic Control Covering Signs Type II	EACH	4.000	4.000
0078	643.1050	Traffic Control Signs PCMS	DAY	7.000	7.000
0080	643.5000	Traffic Control	EACH	0.500	0.500
0082	646.1020	Marking Line Epoxy 4-Inch	LF	101,938.000	101,938.000
0084	648.0100	Locating No-Passing Zones	MI	5.650	5.650
0086	650.8000	Construction Staking Resurfacing Reference	LF	29,825.000	29,825.000
0090	650.9911	Construction Staking Supplemental Control (project) 02. 5908-00-75	EACH	1.000	1.000
0092	690.0150	Sawing Asphalt	LF	497.000	497.000
0094	740.0440	Incentive IRI Ride	DOL	21,667.000	21,667.000
0102	SPV.0195	Special 01. Base Aggregate Dense 3/4-Inch Beam Guard	TON	41.000	41.000

**REMOVING ASPHALTIC PAVEMENT**

204.0115 204.0120  
 REMOVING REMOVING  
 ASPHALTIC SURFACE ASPHALTIC SURFACE  
 BUTT JOINTS MILLING

STATION	LOCATION	SY	SY	REMARKS
Mortimer Rd	LT	76	214	-
CTH Q	LT	183	437	-
Miller Rd	RT	70	206	-
Roebers Dr	RT	81	120	-
Stifter Rd	LT	65	133	-
Shore Rd	RT	80	166	-
David Dr	LT	79	73	-
Hagemann Rd	RT	80	176	-
CTH G	LT	62	291	-
CTH Y	LT	184	0	-
Magruder Rd	RT	64	232	-
814+59 - 816+59	LT&RT	571	0	B-56-157
816+93 - 818+93	LT&RT	578	0	B-56-157
861+73 - 864+83	LT&RT	1067	0	B-56-202
866+57 - 869+32	LT&RT	954	0	B-56-202
877+75 - 879+75	LT&RT	489	0	END OF PROJECT
SUBTOTAL		4682	2047	
630+76	RT	5	31	ASPH, PE
690+97	LT	5	28	ASPH, PE
762+47	LT	4	20	ASPH, PE
766+25	LT	5	25	ASPH, PE
784+19	LT	8	45	ASPH, PE
805+89	LT	4	24	ASPH, PE
824+03	LT	6	39	ASPH, PE
834+41	LT	4	19	ASPH, PE
840+23	LT	4	22	ASPH, PE
845+92	LT	3	17	ASPH, FE
850+12	LT	3	18	ASPH, PE
SUBTOTAL		52	289	
PROJECT TOTALS		4734	2336	

**PREPARE FOUNDATION FOR CIR BASE LAYER**

211.0700.S  
 (5908-00-75)

LOCATION	EACH	NOTES
PROJECT	1	-
PROJECT TOTALS	1	

**FINISHING ROADWAY**

213.0100  
 (5908-00-75)

LOCATION	EACH	NOTES
PROJECT	1	-
PROJECT TOTALS	1	

**DRIVEWAY ITEMS**

\*305.0110 465.0120  
 BASE ASPHALTIC SURFACE  
 AGGREGATE DRIVEWAYS AND  
 DENSE 3/4-INCH FIELD ENTRANCES

STATION	LOCATION	(TON)	(TON)	REMARKS
585+07	LT	1	-	AGG, FE
599+88	RT	1	-	AGG, FE
610+48	LT	2	-	AGG, PE
628+00	LT	2	-	AGG, FE
630+76	RT	-	6	ASPH, PE
632+65	RT	2	-	AGG, FE
633+53	LT	3	-	AGG, FE
643+62	RT	2	-	AGG, FE
643+89	LT	2	-	AGG, FE
674+49	LT	2	-	AGG, FE
688+54	RT	2	-	AGG, PE
690+97	LT	-	5	ASPH, PE
712+09	LT	6	-	AGG, PE
723+46	RT	3	-	AGG, PE
755+96	LT	3	-	AGG, PE
761+18	RT	1	-	AGG, FE
761+23	LT	3	-	AGG, PE
762+47	LT	-	4	ASPH, PE
776+25	LT	-	5	ASPH, PE
784+19	LT	-	9	ASPH, PE
793+90	RT	2	-	AGG, PE
803+77	LT	2	-	AGG, PE
804+87	LT	2	-	AGG, PE
805+89	LT	-	5	ASPH, PE
822+16	LT	2	-	AGG, PE
824+03	LT	-	8	ASPH, PE
831+05	LT	2	-	AGG, PE
833+91	RT	1	-	AGG, PE
834+41	LT	-	4	ASPH, PE
834+59	LT	-	2	AGG, PE
835+82	RT	3	-	AGG, PE
839+31	LT	1	-	AGG, PE
839+50	RT	1	-	AGG, PE
840+23	LT	-	4	ASPH, PE
844+65	RT	2	-	AGG, PE
845+77	RT	1	-	AGG, PE
845+92	LT	-	3	ASPH, PE
850+12	LT	-	4	ASPH, PE
853+06	LT	3	-	AGG, PE
858+80	RT	4	-	AGG, PE
SUBTOTALS		64	59	
PROJECT TOTALS		64	59	

\*BID ITEMS ARE UNDER CATEGORY 0010, UNLESS OTHERWISE NOTED

\*ADDITIONAL QUANTITIES SHOWN ELSEWHERE

**BASE AGGREGATE DENSE**

STATION - STATION	LOCATION	*305.0110 BASE AGGREGATE DENSE 3/4-INCH	SPV.0195.01 BASE AGGREGATE DENSE 3/4-INCH BEAM GUARD	REMARKS
		TON	TON	
581+50 - 651+75	LT&RT	1069	-	-
651+75 - 750+50	LT&RT	1511	-	-
750+50 - 816+59	LT&RT	977	2	-
816+94 - 864+83	LT&RT	672	21	-
866+57 - 879+75	LT&RT	176	17	-
SUBTOTALS		4406	41	
CTH Q	LT	21	-	-
CTH Y	LT	8	-	-
SUBTOTAL		29	0	
<b>PROJECT TOTALS</b>		<b>4435</b>	<b>41</b>	

\*ADDITIONAL QUANTITIES SHOWN ELSEWHERE IN PLANS

**ADJUSTING STEEL PLATE BEAM GUARD**

STATION - STATION	LOCATION	614.0400 ADJUSTING STEEL PLATE BEAM GUARD	REMARKS
		LF	
863+94 - 865+16	LT	12.5	
861+73 - 864+94	RT	200.0	
866+49 - 867+71	LT	25.0	
866+23 - 869+32	RT	175.0	
<b>PROJECT TOTALS</b>		<b>412.5</b>	

**MAINTENANCE AND REPAIR OF HAUL ROADS**

LOCATION	CATEGORY	618.0100 (5908-00-75)
		EACH
PROJECT	0020	1
<b>PROJECT TOTALS</b>		<b>1</b>

**MOBILIZATION**

LOCATION	EACH	619.1000 MOBILIZATION
		NOTES
PROJECT	0.5	PRORATED BETWEEN 5908-00-74
<b>PROJECT TOTALS</b>		<b>0.5</b>

\*BID ITEMS ARE UNDER CATEGORY 0010, UNLESS OTHERWISE NOTED

**ASPHALTIC ITEM SUMMARY**

STATION	LOCATION	211.0800.S BASE REPAIR FOR CIR	327.1000.S COLD IN-PLACE RECYCLING (CIR)	455.0605 TACK	455.0770.S ASPHALT STABILIZING	460.6223 HMA PAVEMENT 3 MT 58-28 S	460.6224 HMA PAVEMENT 4 MT 58-28 S	REMARKS
		LAYER	ASPHALTIC BASE LAYER	COAT	AGENT	TON	TON	
581+50 - 651+75	ML	114	17172	2061	77	-	3374	-
651+75 - 750+50	ML	161	24139	2897	108	-	4735	-
750+50 - 816+59	ML	104	15544	1949	70	-	3189	-
816+94 - 864+83	ML	69	10338	1453	46	-	2361	-
866+57 - 879+75	ML	17	2550	420	11	-	687	-
SUBTOTAL		465	69742	8358	312	0	14346	
Mortimer Rd	LT	-	-	35	-	53	28	-
CTH Q	LT	-	-	74	-	111	61	-
Miller Rd	RT	-	-	33	-	51	27	-
Roebers Dr	RT	-	-	24	-	34	20	-
Stifter Rd	LT	-	-	24	-	35	19	-
Shore Rd	RT	-	-	30	-	43	24	-
David Dr	LT	-	-	18	-	24	15	-
Hagemann Rd	RT	-	-	31	-	45	25	-
CTH G	LT	-	-	42	-	68	35	-
CTH Y	LT	-	-	22	-	19	18	-
Magruder Rd	RT	-	-	35	-	55	29	-
INTERSECTIONS		0	0	368	0	537	301	
<b>PROJECT TOTAL</b>		<b>465</b>	<b>69742</b>	<b>8727</b>	<b>312</b>	<b>537</b>	<b>14647</b>	

**PWL MIXTURE USE TABLE**

LOCATION	STATION - STATION	MIXTURE USE	UNDERLYING SURFACE	BID ITEM	TONS	THICKNESS	MIXTURE ACCEPTANCE	DENSITY ACCEPTANCE
22-FOOT DRIVING LANE	261+41 - 581+50	LOWER LAYER	CIR PAVEMENT	4 MT 58-28 S	7173	1.75"	PWL INCENTIVE AIR VOIDS HMA PAVEMENT 460.2010	INCENTIVE DENSITY PWL HMA PAVEMENT 460.2005
22-FOOT DRIVING LANE	261+41 - 581+50	UPPER LAYER	4 MT 58-28 S	4 MT 58-28 S	7173	1.75"	PWL INCENTIVE AIR VOIDS HMA PAVEMENT 460.2010	INCENTIVE DENSITY PWL HMA PAVEMENT 460.2005
INTERSECTIONS	261+41 - 581+50	LOWER LAYER	MILLED EXISTING HMA SURFACE	3 MT 58-28 S	537	3.75"	QMP AS PER SS 460	INCENTIVE DENSITY HMA 460.2000
INTERSECTIONS	261+41 - 581+50	UPPER LAYER	3 MT 58-28 S	4 MT 58-28 S	301	1.75"	PWL INCENTIVE AIR VOIDS HMA PAVEMENT 460.2010	ACCEPTANCE TESTING BY THE DEPARTMENT; NOT ELIGIBLE FOR INCENTIVE



PERMANENT SIGNING

SIGN NUMBER	LT/RT	SIGN CODE	SIGN MESSAGE	SIZE W" X H"	637.2210	637.2230	634.0612	634.0614	634.0616	634.0618	REMARKS
					SIGNS TYPE II	SIGNS TYPE II	POSTS WOOD	POSTS WOOD	POSTS WOOD	POSTS WOOD	
					REFLECTIVE H (SF)	REFLECTIVE F (SF)	4X6-INCH, 12-FT (EACH)	4X6-INCH, 14-FT (EACH)	4X6-INCH, 16-FT (EACH)	4X6-INCH, 18-FT (EACH)	
01-01	RT	M1-5A	CTH G	24"x24"	4.00	-	-	-	1	-	
01-02	RT	W1-2R	ROADWAY CURVES RIGHT	30"x30"	-	6.25	-	-	-	1	
01-03	RT	W14-3	NO PASSING ZONE	48"x36"	-	12.00	-	-	1	-	
01-04	LT	J1-1	JCT CTH EE	24"x39"	6.50	-	-	-	1	-	
01-05	LT	W1-2L	ROADWAY CURVES LEFT	30"x30"	-	6.25	-	-	-	1	
01-06	LT	W14-3	NO PASSING ZONE	48"x36"	-	12.00	-	-	1	-	
03-01	RT	W14-3	NO PASSING ZONE	48"x36"	-	12.00	-	-	1	-	
03-02	LT	W1-2L	ROADWAY CURVES LEFT	30"x30"	-	6.25	-	-	-	1	
05-01	LT	W14-3	NO PASSING ZONE	48"x36"	-	12.00	-	-	1	-	
06-01	RT	W7-1	STEEP GRADE	30"x30"	-	6.25	-	-	-	1	
06-02	RT	J1-1	JCT CTH Q	24"x39"	6.50	-	-	-	1	-	
06-03	RT	W2-1	FOUR WAY INTERSECTION	30"x30"	-	6.25	-	-	-	1	
06-04	RT	W13-1	45 MPH	18"x18"	-	2.25	-	-	-	-	
06-05	RT	J13-2	CTH Q LEFT / CTH G THRU	48"x45"	15.00	-	-	-	-	1	
06-06	LT	W7-1	STEEP GRADE	30"x30"	-	6.25	-	-	-	1	
07-01	RT	W1-2R	ROADWAY CURVES RIGHT	30"x30"	-	6.25	-	-	-	1	
07-02	LT	W2-1	FOUR WAY INTERSECTION	30"x30"	-	6.25	-	-	-	1	
07-03	LT	W13-1	45 MPH	18"x18"	-	2.25	-	-	-	-	
07-04	LT	J13-2	CTH G THRU / CTH Q RIGHT	48"x45"	15.00	-	-	-	-	1	
07-05	LT	J1-1	JCT CTH Q	24"x39"	6.50	-	-	-	1	-	
08-01	RT	W14-3	NO PASSING ZONE	48"x36"	-	12.00	-	-	1	-	
08-02	LT	W1-2L	ROADWAY CURVES LEFT	30"x30"	-	6.25	-	-	-	1	
10-01	LT	W14-3	NO PASSING ZONE	48"x36"	-	12.00	-	-	1	-	
10-02	RT	W1-2R	ROADWAY CURVES RIGHT	30"x30"	-	6.25	-	-	-	1	
10-03	RT	W13-1	50 MPH	18"x18"	-	2.25	-	-	-	-	
12-01	RT	W1-2L	ROADWAY CURVES LEFT	30"x30"	-	6.25	-	-	-	1	
12-02	RT	W13-1	35 MPH	18"x18"	-	2.25	-	-	-	-	
12-03	LT	W1-2L	ROADWAY CURVES LEFT	30"x30"	-	6.25	-	-	-	1	
12-04	LT	W13-1	50 MPH	18"x18"	-	2.25	-	-	-	-	
12-05	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-	
12-06	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-	
12-07	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-	
12-08	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-	
12-09	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-	
12-10	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-	
12-11	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-	
12-12	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-	
12-13	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-	
12-14	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-	
13-01	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-	
13-02	RT	W1-2R	ROADWAY CURVES RIGHT	30"x30"	-	6.25	-	-	-	1	
13-03	RT	W13-1	50 MPH	18"x18"	-	2.25	-	-	-	-	
13-04	LT	W1-2R	ROADWAY CURVES RIGHT	30"x30"	-	6.25	-	-	-	1	
13-05	LT	W13-1	35 MPH	18"x18"	-	2.25	-	-	-	-	
13-06	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-	

CONTINUED

WATER

624.0100

WATER

STATION	LOCATION	MGAL	REMARKS
581+50 - 651+75	ML	49	CIR AND SHOULDER
651+75 - 750+50	ML	69	CIR AND SHOULDER
750+50 - 816+59	ML	44	CIR AND SHOULDER
816+94 - 864+83	ML	30	CIR AND SHOULDER
866+57 - 879+75	ML	8	CIR AND SHOULDER
SUBTOTAL		199	
PROJECT TOTAL		199	

\*BID ITEMS ARE UNDER CATEGORY 0010, UNLESS OTHERWISE NOTED

PERMANENT SIGNING (CONT'D)

SIGN NUMBER	LT/RT	SIGN CODE	SIGN MESSAGE	SIZE W" X H"	637.2210	637.2230	634.0612	634.0614	634.0616	634.0618
					SIGNS	SIGNS	POSTS	POSTS	POSTS	POSTS
					TYPE II	TYPE II	WOOD	WOOD	WOOD	WOOD
13-07	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
13-08	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
13-09	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
13-10	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
13-11	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
13-12	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
13-13	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
13-14	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
14-01	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
14-02	RT	W1-2L	ROADWAY CURVES LEFT	30"x30"	-	6.25	-	-	-	1
14-03	RT	W13-1	45 MPH	18"x18"	-	2.25	-	-	-	-
14-04	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
14-05	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
14-06	LT	W1-2L	ROADWAY CURVES LEFT	30"x30"	-	6.25	-	-	-	1
14-07	LT	W13-1	40 MPH	18"x18"	-	2.25	-	-	-	-
14-08	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
14-09	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
14-10	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
14-11	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
14-12	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
14-13	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
14-14	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
14-15	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
14-16	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
14-17	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
14-18	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
15-01	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
15-02	LT	W1-2L	ROADWAY CURVES RIGHT	30"x30"	-	6.25	-	-	-	1
15-03	LT	W13-1	45 MPH	18"x18"	-	2.25	-	-	-	-
15-04	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
15-05	LT	W7-1	STEEP GRADE	30"x30"	-	6.25	-	-	-	1
16-01	LT	W14-3	NO PASSING ZONE	48"x36"	-	5.56	-	1	-	-
16-02	RT	W14-3	NO PASSING ZONE	48"x36"	-	5.56	-	1	-	-
18-01	RT	J1-1	JCT CTH Y	24"x39"	6.50	-	-	1	-	-
18-02	RT	W1-1R	ROADWAY TURNS RIGHT	36"x36"	-	9.00	-	-	-	1
18-03	RT	W13-1	30 MPH	18"x18"	-	2.25	-	-	-	-
18-04	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
18-05	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
18-06	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
18-07	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
18-08	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
18-09	RT	J13-2	CTH Y LEFT / CTH G DR	48"x45"	15.00	-	-	-	-	1
18-10	LT	J13-2	CTH G DL / CTH Y DR	48"x45"	15.00	-	-	-	-	1

CONTINUED

PERMANENT SIGNING (CONT'D)

SIGN NUMBER	LT/RT	SIGN CODE	SIGN MESSAGE	SIZE W" X H"	637.2210	637.2230	634.0612	634.0614	634.0616	634.0618
					SIGNS	SIGNS	POSTS	POSTS	POSTS	POSTS
					TYPE II	TYPE II	WOOD	WOOD	WOOD	WOOD
18-11	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
18-12	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
18-13	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
18-14	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
18-15	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
18-16	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
18-17	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
18-18	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
18-19	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
18-20	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
18-21	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
19-01	LT	W1-1L	ROADWAY TURNS LEFT	36"x36"	-	9.00	-	-	-	1
19-02	LT	W13-1	30 MPH	18"x18"	-	2.25	-	-	-	-
19-03	RT	W1-1L	ROADWAY TURNS LEFT	36"x36"	-	9.00	-	-	-	1
19-04	RT	W13-1	25 MPH	18"x18"	-	2.25	-	-	-	-
19-05	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
19-06	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
20-01	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
20-02	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
20-03	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
20-04	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
20-05	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
20-06	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
20-07	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
20-08	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
20-09	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
20-10	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
20-11	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
20-12	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
20-13	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
20-14	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
20-15	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
20-16	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
20-17	LT	W5-52L	BRIDGE HAZARD	12"x36"	-	3.00	1	-	-	-
20-18	RT	W5-52R	BRIDGE HAZARD	12"x36"	-	3.00	1	-	-	-
20-19	LT	W5-52R	BRIDGE HAZARD	12"x36"	-	3.00	1	-	-	-
20-20	RT	W5-52L	BRIDGE HAZARD	12"x36"	-	3.00	1	-	-	-
20-21	LT	W1-1R	ROADWAY TURNS RIGHT	36"x36"	-	9.00	-	-	-	1
20-22	LT	W13-1	25 MPH	18"x18"	-	2.25	-	-	-	-
20-23	RT	W1-2R	ROADWAY CURVES RIGHT	30"x30"	-	6.25	-	-	-	1
20-24	RT	W13-1	40 MPH	18"x18"	-	2.25	-	-	-	-
20-25	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
20-26	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
20-27	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-

CONTINUED

\*BID ITEMS ARE UNDER CATEGORY 0010, UNLESS OTHERWISE NOTED



PERMANENT SIGNING (CONT'D)

SIGN NUMBER	LT/RT	SIGN CODE	SIGN MESSAGE	SIZE W" X H"	637.2210	637.2230	634.0612	634.0614	634.0616	634.0618
					SIGNS	SIGNS	POSTS	POSTS	POSTS	POSTS
					TYPE II	TYPE II	WOOD	WOOD	WOOD	WOOD
					REFLECTIVE H (SF)	REFLECTIVE F (SF)	4x6-INCH, 12-FT (EACH)	4x6-INCH, 14-FT (EACH)	4x6-INCH, 16-FT (EACH)	4x6-INCH, 18-FT (EACH)
20-28	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
21-01	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
21-02	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
21-03	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
21-04	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
21-05	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
21-06	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
21-07	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
21-08	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
21-09	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
21-10	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
21-11	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
21-12	RT	W1-4R	URNS IN ROADWAY	36"x36"	-	9.00	-	-	-	1
21-13	RT	W13-1	35 MPH	18"x18"	-	2.25	-	-	-	-
21-14	LT	W1-2L	ROADWAY CURVES LEFT	30"x30"	-	6.25	-	-	-	1
21-15	LT	W13-1	40 MPH	18"x18"	-	2.25	-	-	-	-
21-16	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
22-01	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
22-02	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
22-03	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
22-04	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
22-05	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
22-06	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
22-07	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
22-08	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
22-09	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
22-10	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
22-11	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
22-12	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
22-13	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
22-14	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
22-15	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
22-16	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
22-17	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
22-18	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
22-19	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
22-20	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
23-01	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
23-02	RT	W1-5L	CURVES IN ROADWAY	30"x30"	-	6.25	-	-	1	-
23-03	RT	W13-1	40 MPH	18"x18"	-	2.25	-	-	-	-
23-04	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
23-05	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
23-06	LT	W1-4L	CURVES IN ROADWAY	36"x36"	-	9.00	-	-	-	1
23-07	LT	W13-1	35 MPH	18"x18"	-	2.25	-	-	-	-
23-08	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-

CONTINUED

\*BID ITEMS ARE UNDER CATEGORY 0010, UNLESS OTHERWISE NOTED

PERMANENT SIGNING (CONT'D)

SIGN NUMBER	LT/RT	SIGN CODE	SIGN MESSAGE	SIZE W" X H"	637.2210	637.2230	634.0612	634.0614	634.0616	634.0618
					SIGNS	SIGNS	POSTS	POSTS	POSTS	POSTS
					TYPE II	TYPE II	WOOD	WOOD	WOOD	WOOD
					REFLECTIVE H (SF)	REFLECTIVE F (SF)	4x6-INCH, 12-FT (EACH)	4x6-INCH, 14-FT (EACH)	4x6-INCH, 16-FT (EACH)	4x6-INCH, 18-FT (EACH)
23-09	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
23-10	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
23-11	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
23-12	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
23-13	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
23-14	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
23-15	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
23-16	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
23-17	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
23-18	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
24-01	LT	W1-2L	ROADWAY CURVES LEFT	30"x30"	-	6.25	-	-	-	1
24-02	LT	W13-1	45 MPH	18"x18"	-	2.25	-	-	-	-
25-01	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
25-02	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
25-03	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
25-04	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
25-05	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
25-06	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
25-07	RT	I2-2	JUNEAU CO	54"x15"	5.60	-	-	1	-	-
25-08	RT	M1-5A	CTH G	24"x24"	4.00	-	-	-	-	1
25-09	RT	D11-10	ATV ROUTE	24"x18"	3.00	-	-	-	-	-
25-10	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
25-11	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
25-12	LT	I2-2	SAUK CO	46"x15"	4.80	-	-	1	-	-
25-13	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	1	-	-
25-14	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
25-15	LT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
25-16	LT	M1-5A	CTH G	24"x24"	4.00	-	-	-	1	-
25-17	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
25-18	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
25-19	RT	W1-8	CHEVRON	18"x24"	-	3.00	-	-	-	-
<b>PROJECT TOTALS</b>					<b>111.40</b>	<b>687.73</b>	<b>4</b>	<b>66</b>	<b>15</b>	<b>33</b>

**REMOVING SIGNS**

SIGN #	SIGN MESSAGE	LOCATION	638.2602	638.3000	REMARKS
			REMOVING SIGNS TYPE II (EACH)	REMOVING SMALL SIGN SUPPORTS (EACH)	
R01-01	NO PASSING ZONE	CTH G	1	1	
R01-02	CTH G	CTH G	1	1	
R01-03	VALTON RIGHT	CTH G	-	-	TO BE REMOVED BY COUNTY BEFORE CONSTRUCTION
R01-04	JCT CTH EE	CTH G	1	-	
R01-05	NO PASSING ZONE	CTH G	1	-	
R01-06	ROADWAY CURVES LEFT	CTH G	1	-	
R03-01	NO PASSING ZONE	CTH G	1	1	
R03-02	ROADWAY CURVES LEFT	CTH G	1	1	
R05-01	NO PASSING ZONE	CTH G	1	1	
R06-01	STEEP GRADE	CTH G	1	1	
R06-02	JCT CTH Q	CTH G	1	1	
R06-03	FOUR WAY INTERSECTION	CTH G	1	1	
R06-04	45 MPH	CTH G	1	-	
R06-05	CTH Q LEFT / CTH G THRU	CTH G	1	1	
R06-06	STEEP GRADE	CTH G	1	1	
R07-01	ROADWAY CURVES RIGHT	CTH G	1	1	
R07-02	JCT CTH Q	CTH G	1	1	
R07-03	FOUR WAY INTERSECTION	CTH G	1	1	
R07-04	45 MPH	CTH G	1	-	
R07-05	CTH G THRU / CTH Q RIGHT	CTH G	1	1	
R08-01	NO PASSING ZONE	CTH G	1	1	
R08-02	ROADWAY CURVES LEFT	CTH G	1	1	
R08-03	45 MPH	CTH G	1	-	
R10-01	NO PASSING ZONE	CTH G	1	1	
R10-02	ROADWAY CURVES RIGHT	CTH G	1	1	
R12-01	ROADWAY CURVES LEFT	CTH G	1	1	
R12-02	ROADWAY CURVES LEFT	CTH G	1	1	
R13-01	CURVES IN ROADWAY	CTH G	1	1	
R13-02	45 MPH	CTH G	1	-	
R13-03	ROADWAY CURVES RIGHT	CTH G	1	1	
R13-04	45 MPH	CTH G	1	-	
R14-01	RIGHT ARROW	CTH G	1	1	
R15-01	STEEP GRADE	CTH G	1	1	
R15-02	CURVES IN ROADWAY	CTH G	1	1	
R15-03	45 MPH	CTH G	1	-	
R16-01	NO PASSING ZONE	CTH G	1	1	
R16-02	ROADWAY CURVES RIGHT	CTH G	1	1	
R16-03	45 MPH	CTH G	1	-	
R16-04	NO PASSING ZONE	CTH G	1	1	
R17-01	ROADWAY TURNS RIGHT	CTH G	1	1	
R17-02	25 MPH	CTH G	1	-	

CONTINUED

**REMOVING SIGNS (CONT'D)**

SIGN #	SIGN MESSAGE	LOCATION	638.2602	638.3000	REMARKS
			REMOVING SIGNS TYPE II (EACH)	REMOVING SMALL SIGN SUPPORTS (EACH)	
R18-01	JCT CTH Y	CTH G	1	1	
R18-02	ROADWAY CURVES LEFT	CTH G	1	1	
R18-03	45 MPH	CTH G	1	-	
R18-04	CTH Y LEFT / CTH G DR	CTH G	1	1	
R18-05	CTH Y DL / CTH G DR	CTH G	1	1	
R18-06	RIGHT ARROW	CTH G	1	1	
R19-01	TURNS IN ROADWAY	CTH G	1	1	
R19-02	30 MPH	CTH G	1	-	
R20-01	BRIDGE HAZARD	CTH G	1	1	
R20-02	BRIDGE HAZARD	CTH G	1	1	
R20-03	BRIDGE HAZARD	CTH G	1	1	
R20-04	BRIDGE HAZARD	CTH G	1	1	
R21-01	CURVES IN ROADWAY	CTH G	1	1	
R21-02	30 MPH	CTH G	1	-	
R22-01	TURNS IN ROADWAY	CTH G	1	1	
R22-02	30 MPH	CTH G	1	-	
R22-03	LEFT ARROW	CTH G	1	1	
R25-01	CTH G	CTH G	1	1	
R25-02	JUNEAU CO	CTH G	1	-	
R25-03	ATV ROUTE	CTH G	1	-	
R25-04	SAUK CO	CTH G	1	1	
R25-05	CTH G	CTH G	1	1	

**PROJECT TOTALS 62 45**

\*BID ITEMS ARE UNDER CATEGORY 0010, UNLESS OTHERWISE NOTED

FIELD OFFICE TYPE C

LOCATION	EACH	NOTES
PROJECT	0.5	-
<b>PROJECT TOTALS</b>	<b>0.5</b>	

TRAFFIC CONTROL

STAGE	LOCATION	643.0420		643.0705		643.0900		**643.0920		643.1050		NOTES
		TRAFFIC CONTROL	TRAFFIC CONTROL	TRAFFIC CONTROL	TRAFFIC CONTROL	COVERING SIGNS	TRAFFIC CONTROL	TRAFFIC CONTROL	TRAFFIC CONTROL			
APPROX. SERVICE PERIOD	BARRICADES TYPE III	WARNING LIGHTS TYPE A	TRAFFIC CONTROL SIGNS	COVERING SIGNS TYPE II	TRAFFIC CONTROL SIGNS PCMS	DAYS	QTY.	DAYS	QTY.	DAYS	QTY.	DAYS
1	PRE-WARNING	7	0	0	0	0	0	0	0	1	7	PRE-WARN PRIOR TO CONSTRUCTION START
	ENTIRE PROJECT	50	24	1200	48	2400	49	2450	4	0	0	
	<b>SUBTOTAL</b>			<b>1200</b>		<b>2400</b>		<b>2450</b>	<b>4</b>		<b>7</b>	
	<b>PROJECT TOTALS</b>			<b>1200</b>		<b>2400</b>		<b>2450</b>	<b>4</b>		<b>7</b>	

\*\*NUMBER OF CYCLES = 1

MARKING LINE ITEMS

STATION - STATION	LOCATION	646.1020				REMARKS
		MARKING LINE EPOXY 4-INCH				
		SOLID WHITE LF	SOLID YELLOW LF	3' LINE 9' SKIP WHITE LF	12.5' LINE 37.5' SKIP YELLOW LF	
581+50 - 651+75	LT&RT	13,767	5,655	30	1,525	
651+75 - 750+50	LT&RT	19,119	12,575	-	1,225	
750+50 - 816+59	LT&RT	12,761	9,374	105	538	
816+59 - 864+83	LT&RT	9,648	9,648	-	-	
864+83 - 879+75	LT&RT	2,984	2,984	-	-	
	<b>SUBTOTAL</b>	<b>58,279</b>	<b>40,236</b>	<b>135</b>	<b>3,288</b>	
	<b>PROJECT TOTAL</b>			<b>101,938</b>		

SAWING ASPHALT

STATION	LOCATION	(LF)	REMARKS
Mortimer Rd	LT	22	-
CTH Q	LT	25	-
Miller Rd	RT	23	-
Roehers Dr	RT	21	-
Stifter Rd	LT	20	-
Shore Rd	RT	23	-
David Dr	LT	20	-
Hagemann Rd	RT	25	-
CTH G	LT	22	-
CTH Y	LT	31	-
Magruder Rd	RT	21	-
879+75	LT&RT	22	END OF PROJECT
<b>SUBTOTAL</b>		<b>275</b>	
630+76	RT	24	ASPH, PE
690+96	LT	21	ASPH, PE
762+46	LT	16	ASPH, PE
776+25	LT	20	ASPH, PE
784+21	LT	36	ASPH, PE
805+91	LT	17	ASPH, PE
824+07	LT	28	ASPH, PE
834+43	LT	16	ASPH, PE
840+25	LT	18	ASPH, PE
845+90	LT	12	ASPH, PE
850+21	LT	14	ASPH, PE
<b>SUBTOTAL</b>		<b>222</b>	
<b>PROJECT TOTALS</b>		<b>497</b>	

TRAFFIC CONTROL

LOCATION	EACH	NOTES
PROJECT	0.5	PRORATED BETWEEN 5908-00-74
<b>PROJECT TOTALS</b>	<b>0.5</b>	

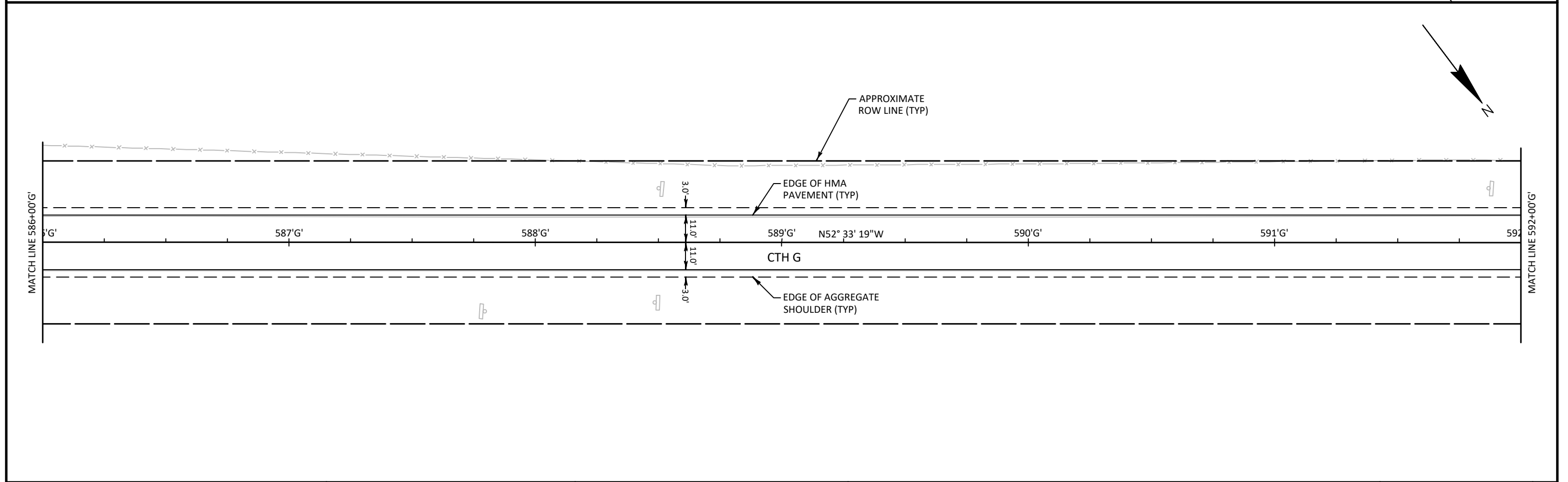
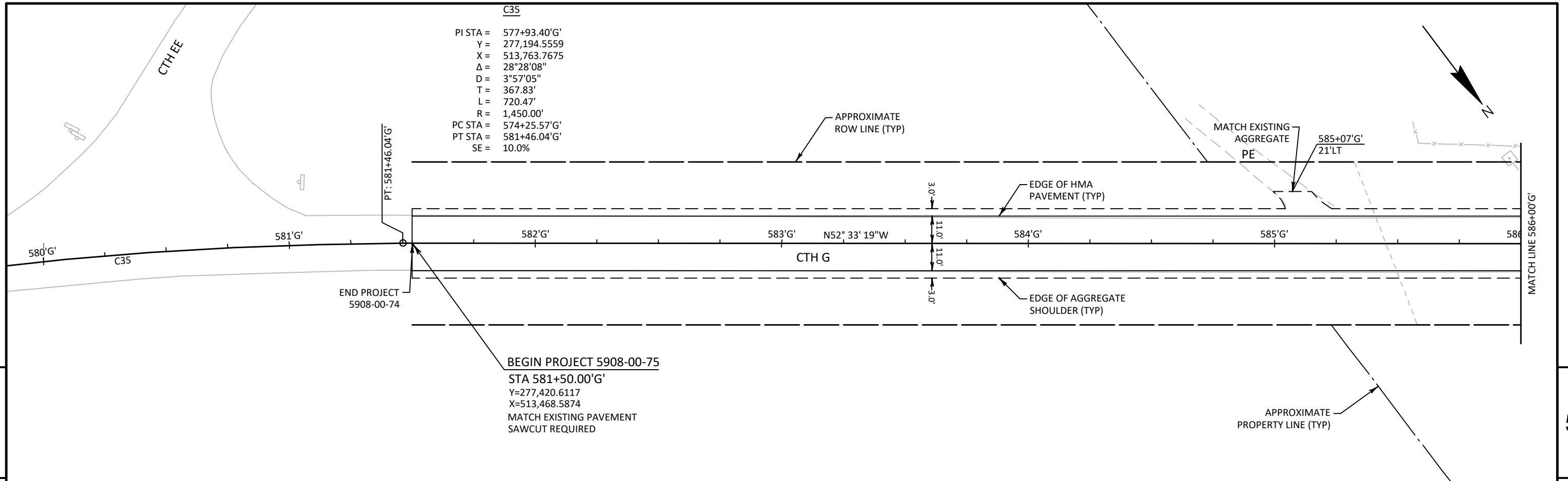
LOCATING NO-PASSING ZONES

LOCATION	MI	NOTES
PROJECT	5.65	-
<b>PROJECT TOTALS</b>	<b>5.65</b>	

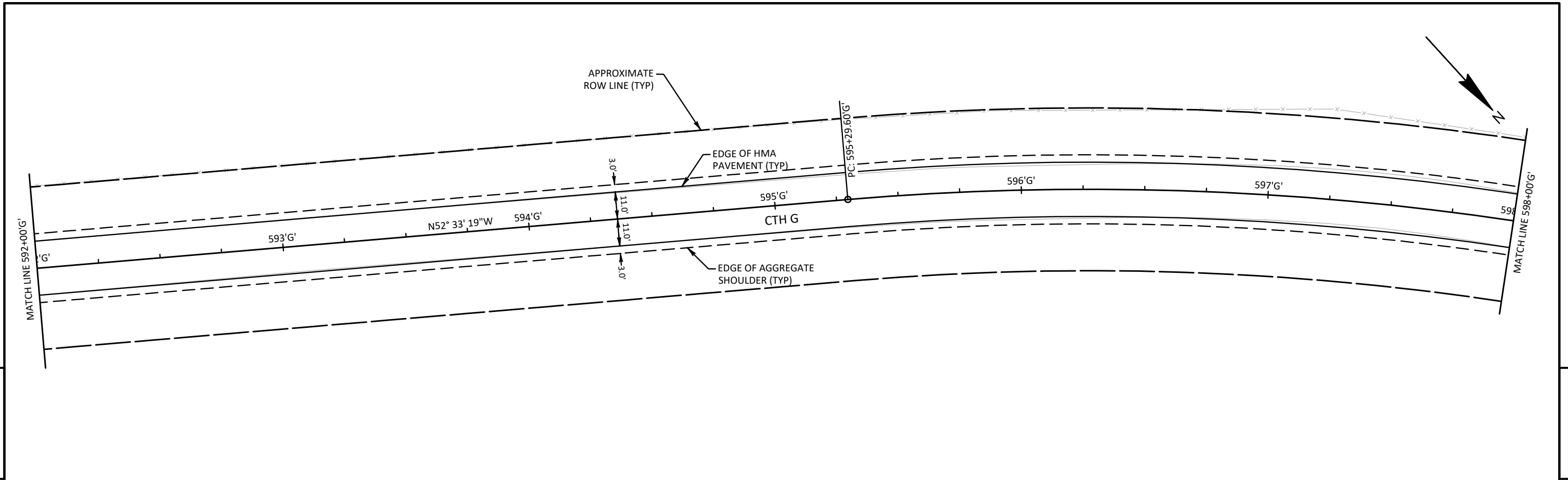
CONSTRUCTION STAKING

STATION	LF	LS	NOTES
581+50 - 879+75	29825	-	-
PROJECT	-	1	-
<b>PROJECT TOTALS</b>	<b>29825</b>	<b>1</b>	

\*BID ITEMS ARE UNDER CATEGORY 0010, UNLESS OTHERWISE NOTED

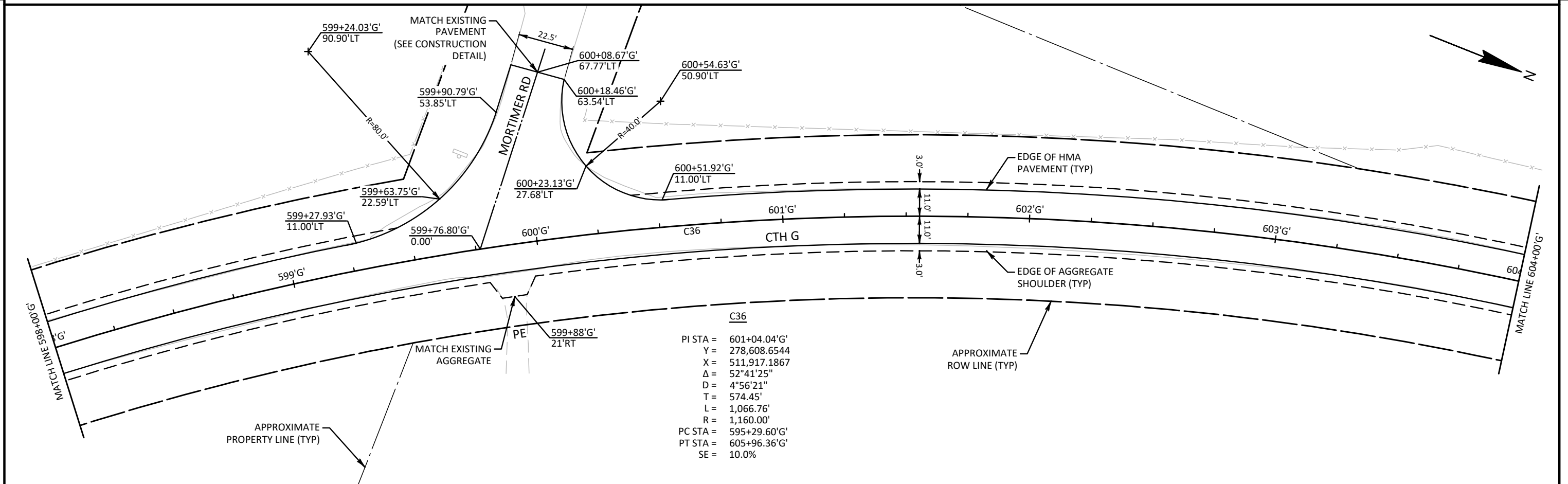


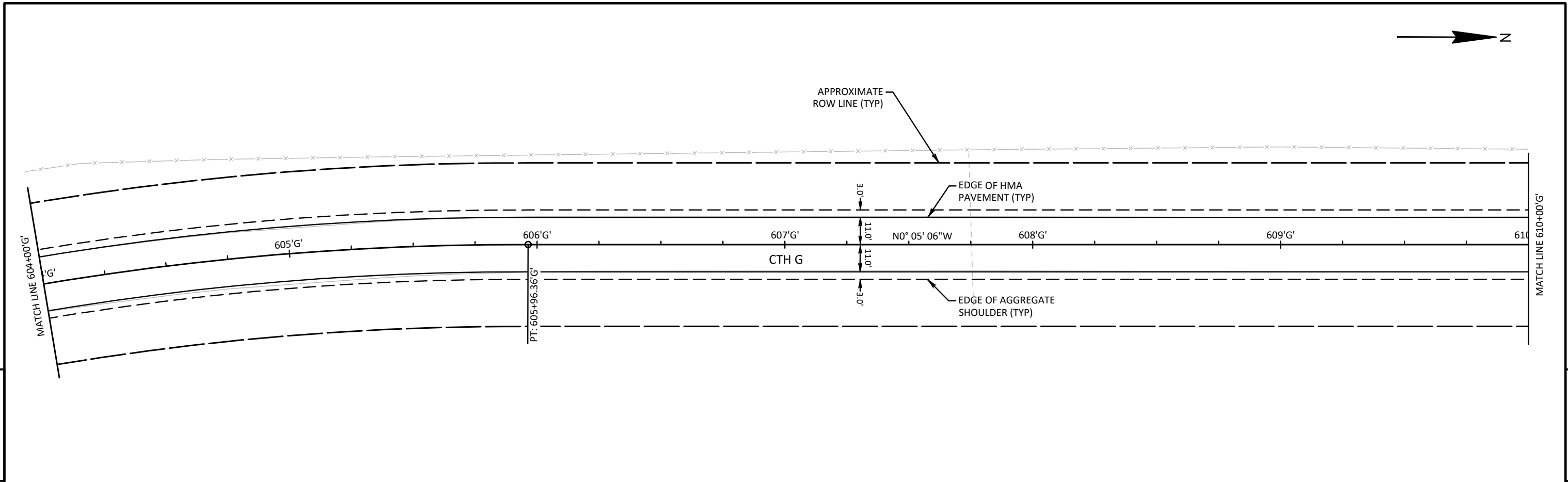
PROJECT NO: 5908-00-75	HWY: CTH G	COUNTY: SAUK	PLAN: CTH G	SHEET	E
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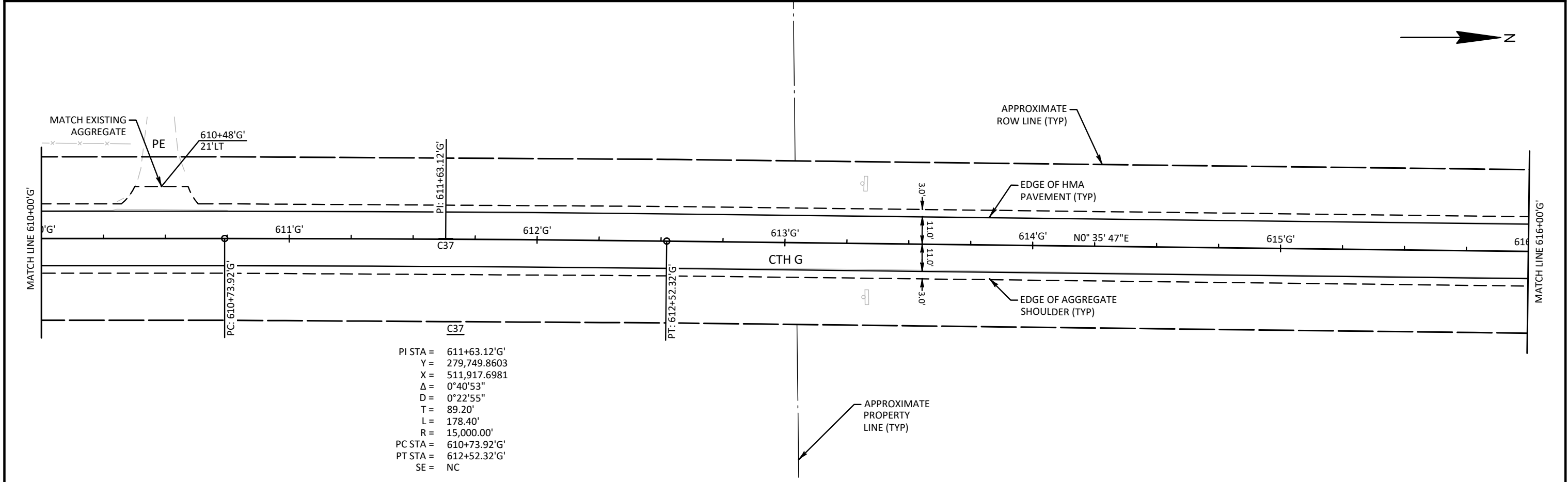
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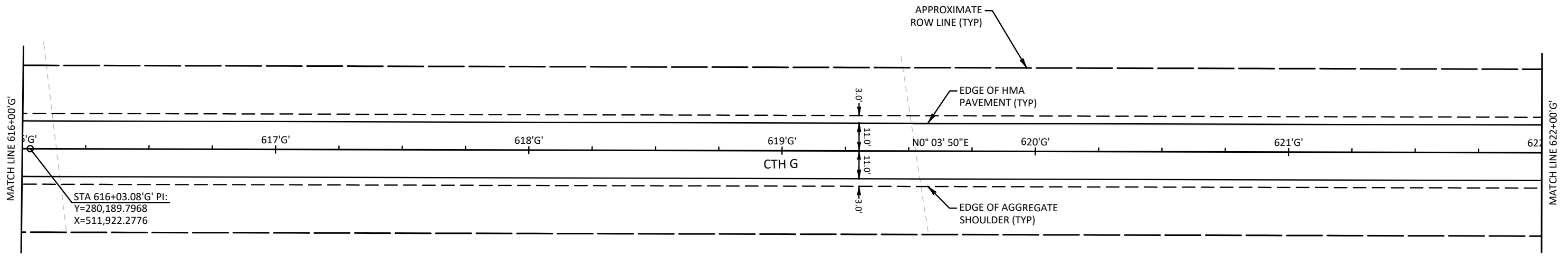
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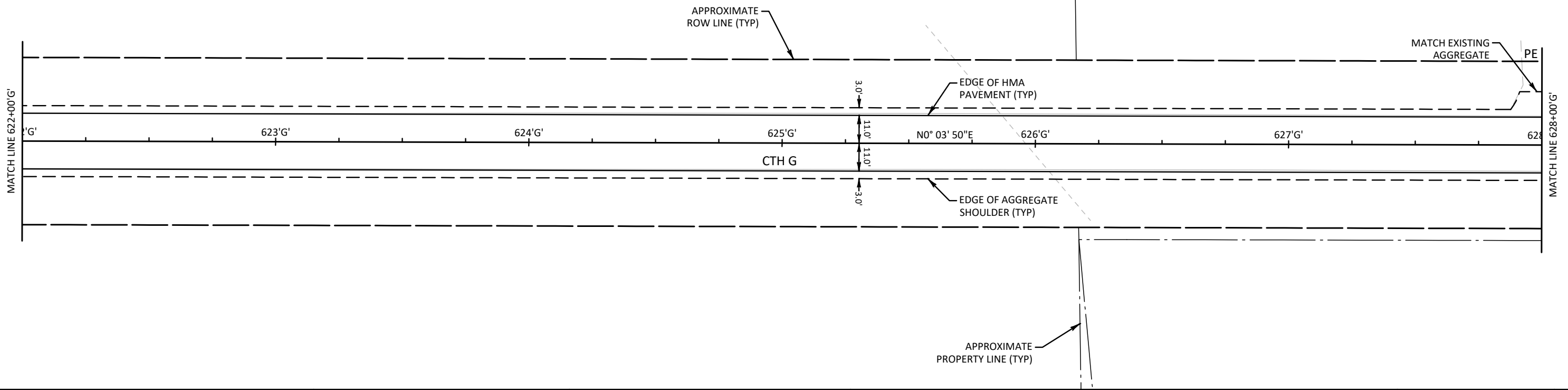
PI STA = 611+63.12'G'  
 Y = 279,749.8603  
 X = 511,917.6981  
 Δ = 0°40'53"  
 D = 0°22'55"  
 T = 89.20'  
 L = 178.40'  
 R = 15,000.00'  
 PC STA = 610+73.92'G'  
 PT STA = 612+52.32'G'  
 SE = NC



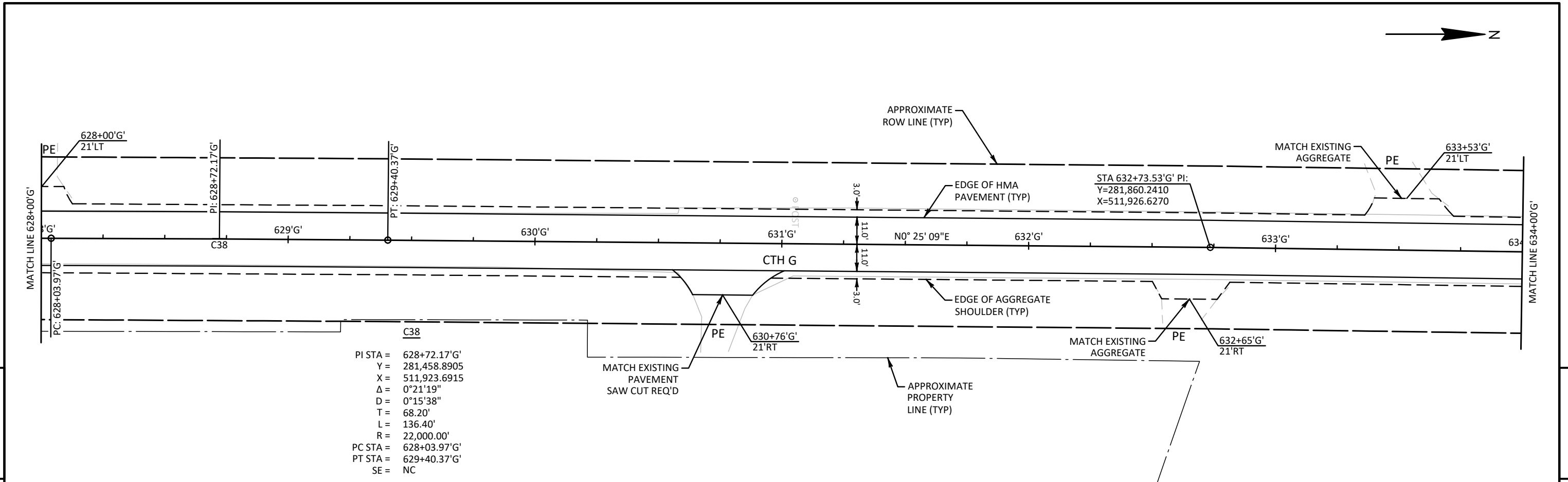


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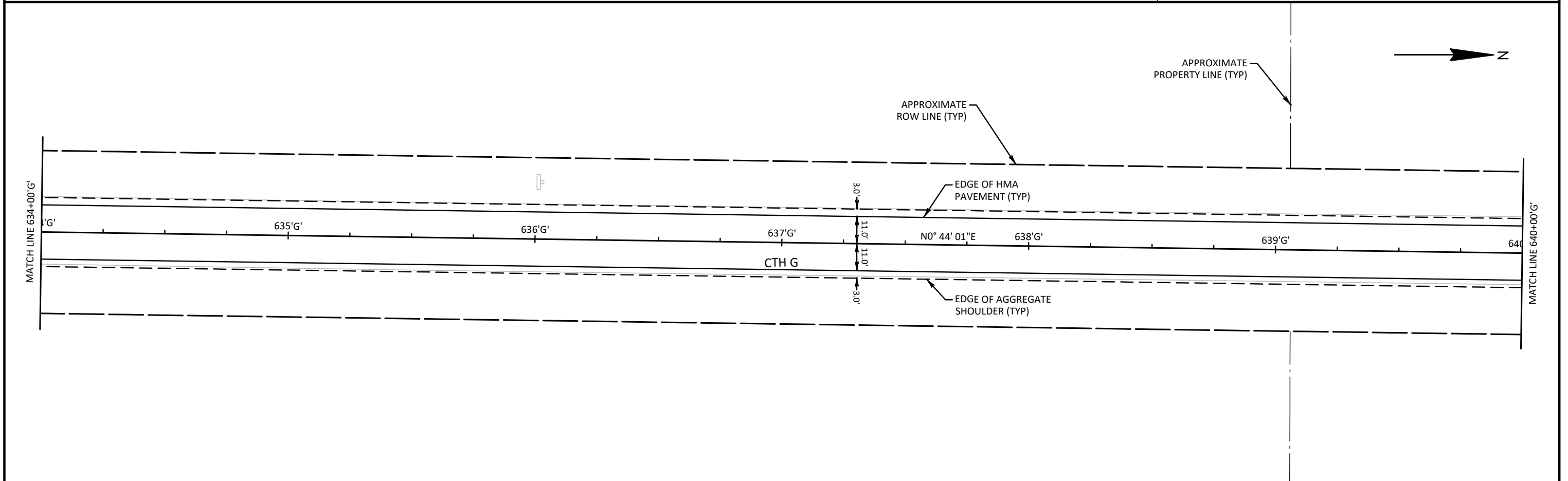
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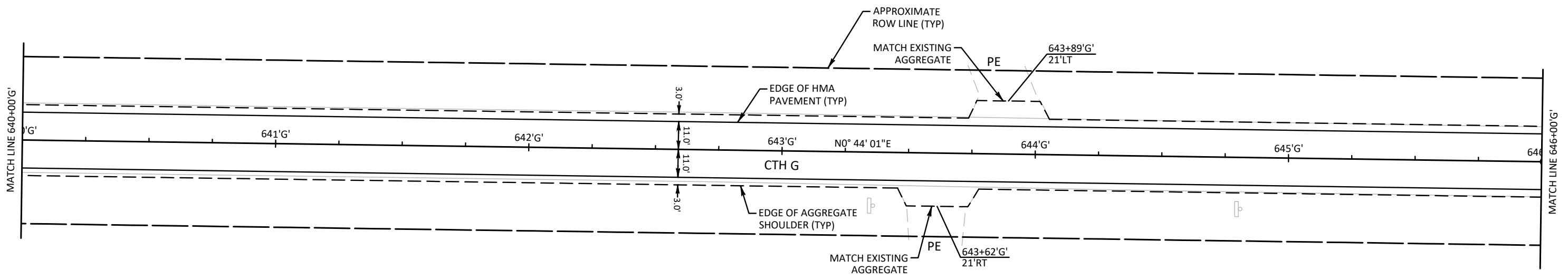
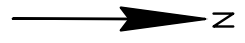


PROJECT NO: 5908-00-75	HWY: CTH G	COUNTY: SAUK	PLAN: CTH G	SHEET	E
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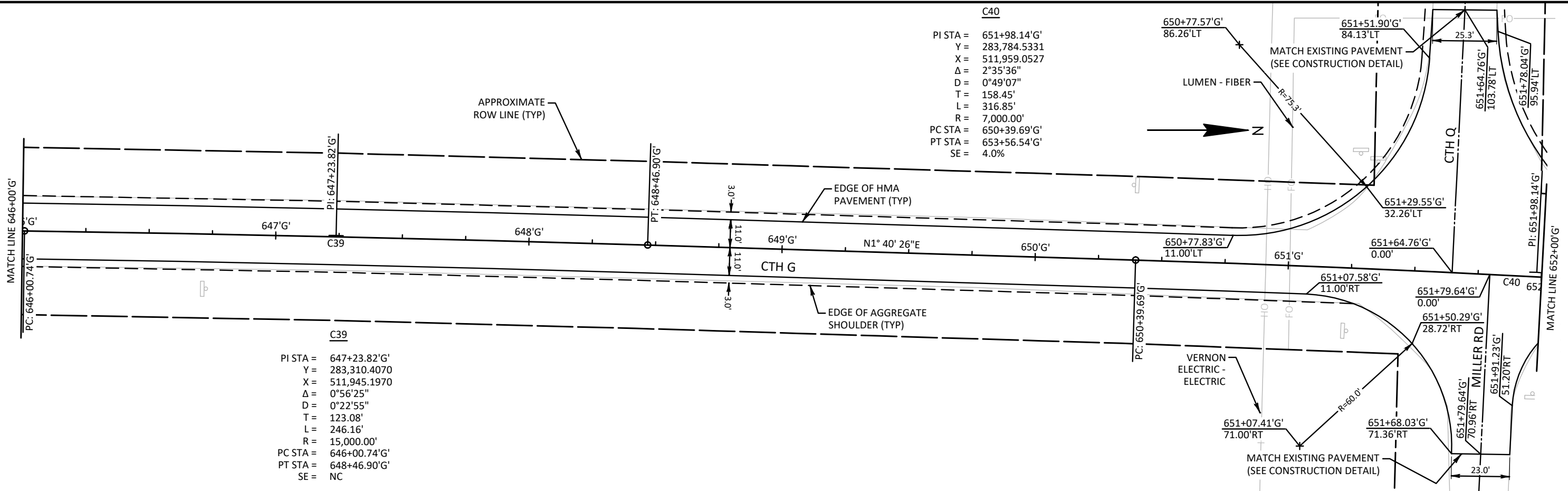
C38  
 PI STA = 628+72.17'G'  
 Y = 281,458.8905  
 X = 511,923.6915  
 Δ = 0°21'19"  
 D = 0°15'38"  
 T = 68.20'  
 L = 136.40'  
 R = 22,000.00'  
 PC STA = 628+03.97'G'  
 PT STA = 629+40.37'G'  
 SE = NC





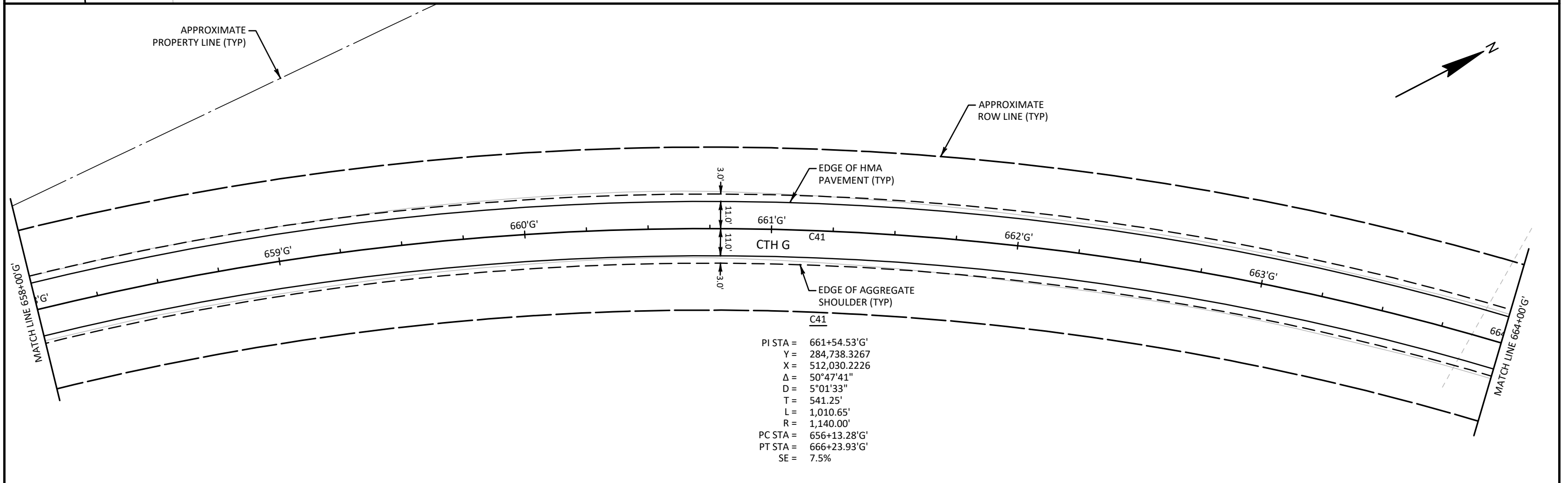
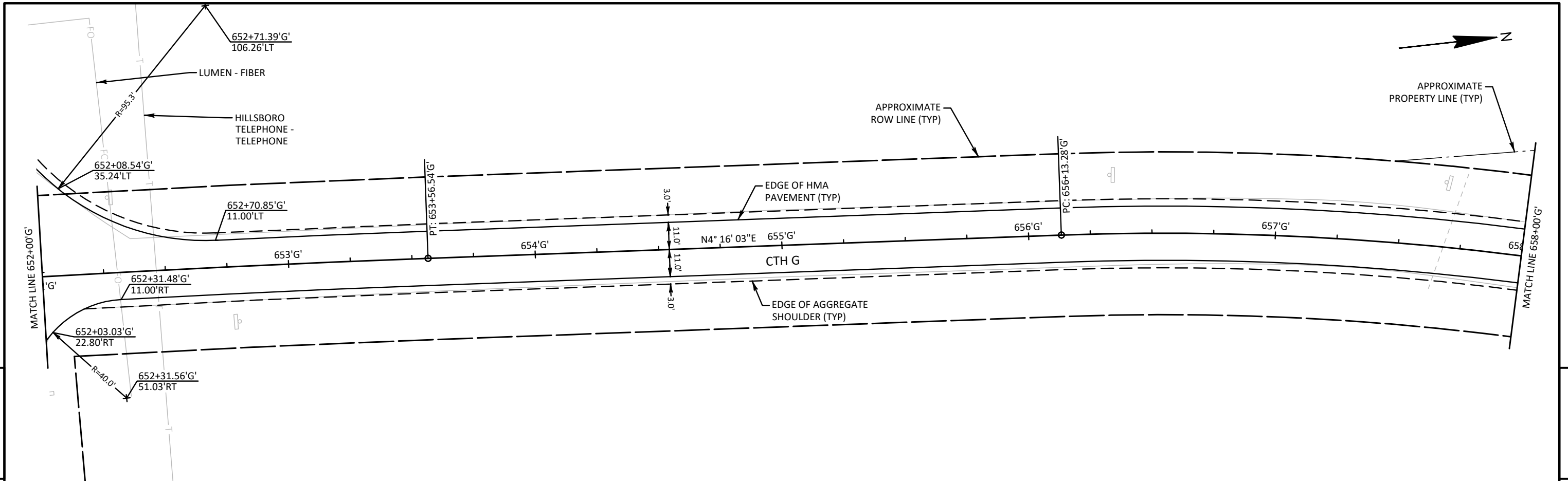
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**C39**  
 PI STA = 647+23.82'  
 Y = 283,310.4070  
 X = 511,945.1970  
 Δ = 0°56'25"  
 D = 0°22'55"  
 T = 123.08'  
 L = 246.16'  
 R = 15,000.00'  
 PC STA = 646+00.74'  
 PT STA = 648+46.90'  
 SE = NC

**C40**  
 PI STA = 651+98.14'  
 Y = 283,784.5331  
 X = 511,959.0527  
 Δ = 2°35'36"  
 D = 0°49'07"  
 T = 158.45'  
 L = 316.85'  
 R = 7,000.00'  
 PC STA = 650+39.69'  
 PT STA = 653+56.54'  
 SE = 4.0%



PROJECT NO: 5908-00-75

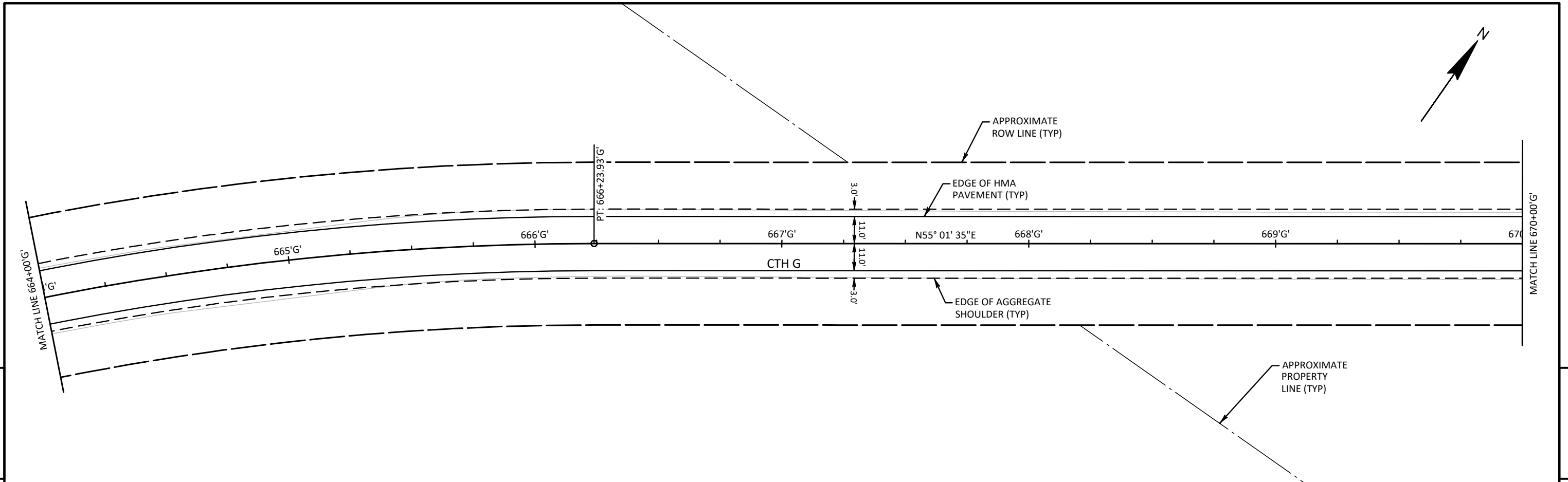
HWY: CTH G

COUNTY: SAUK

PLAN: CTH G

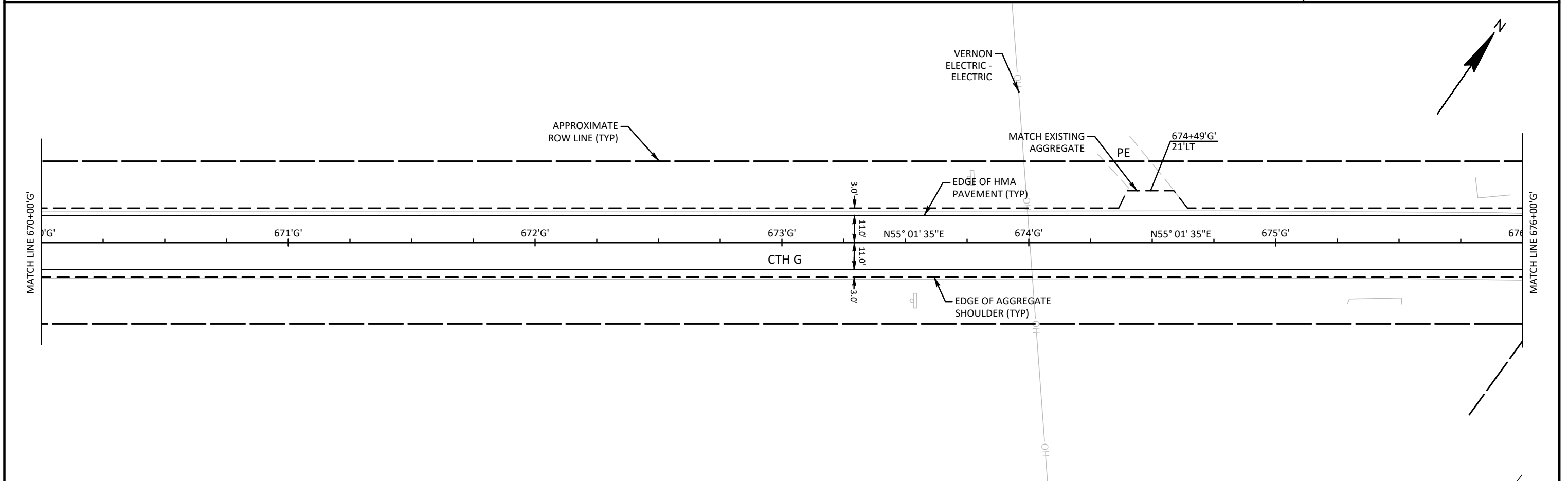
SHEET

E

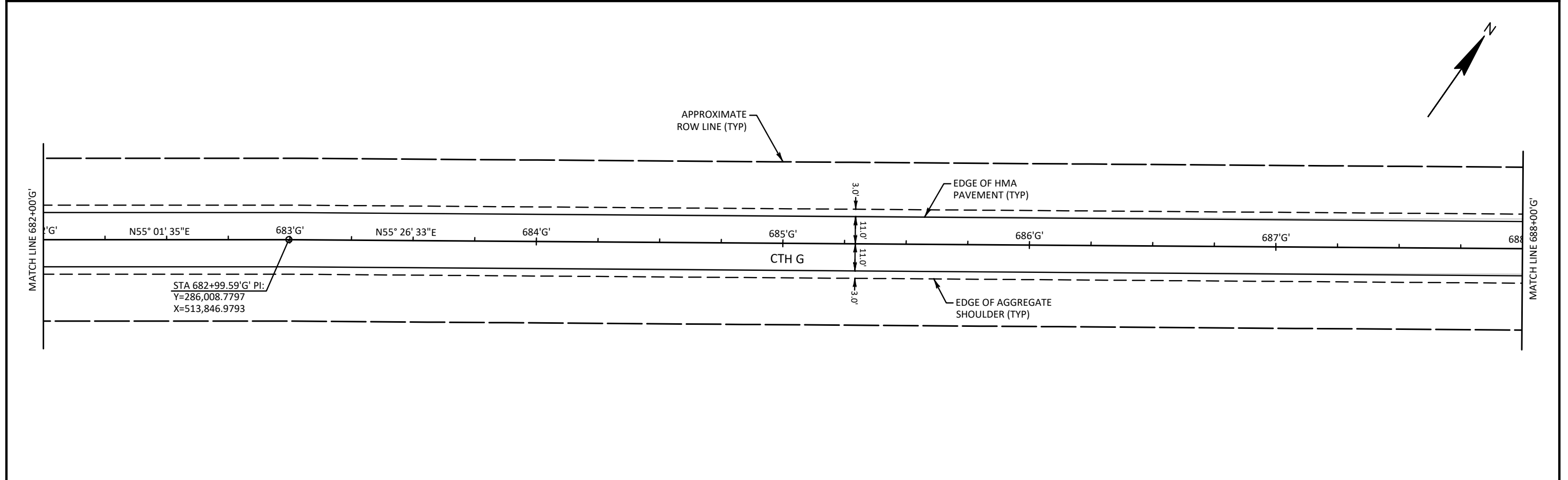
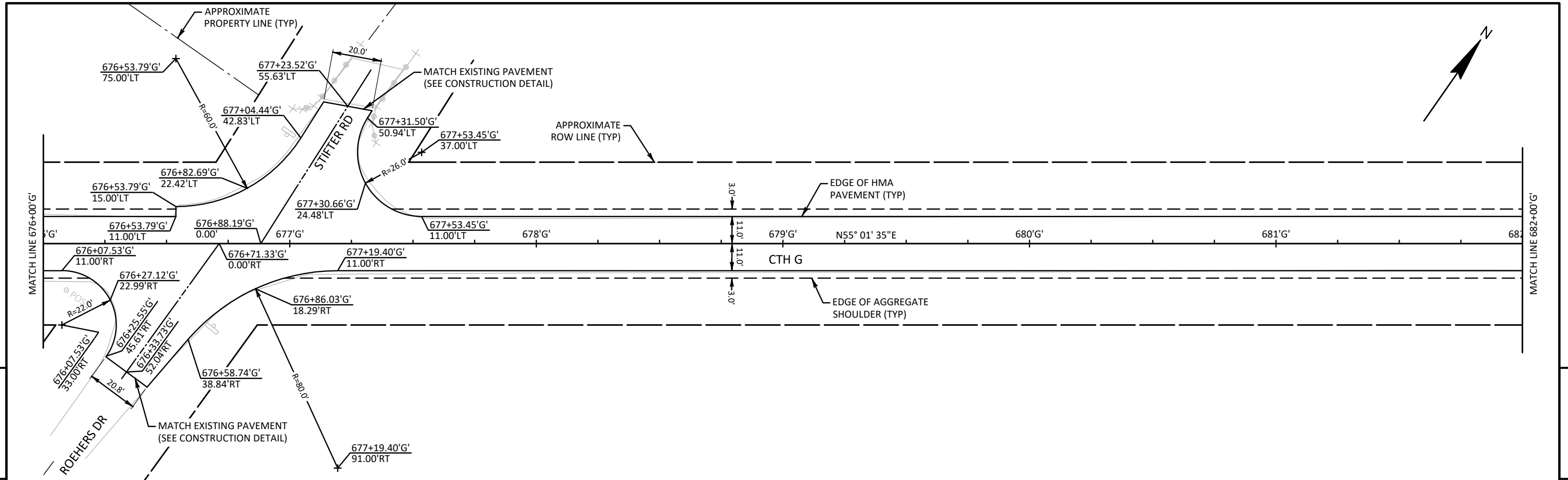


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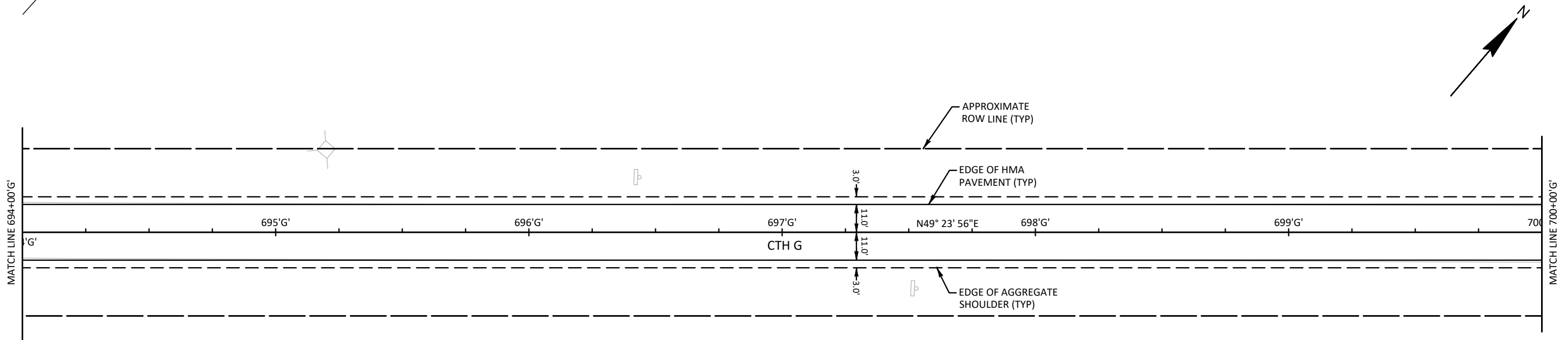
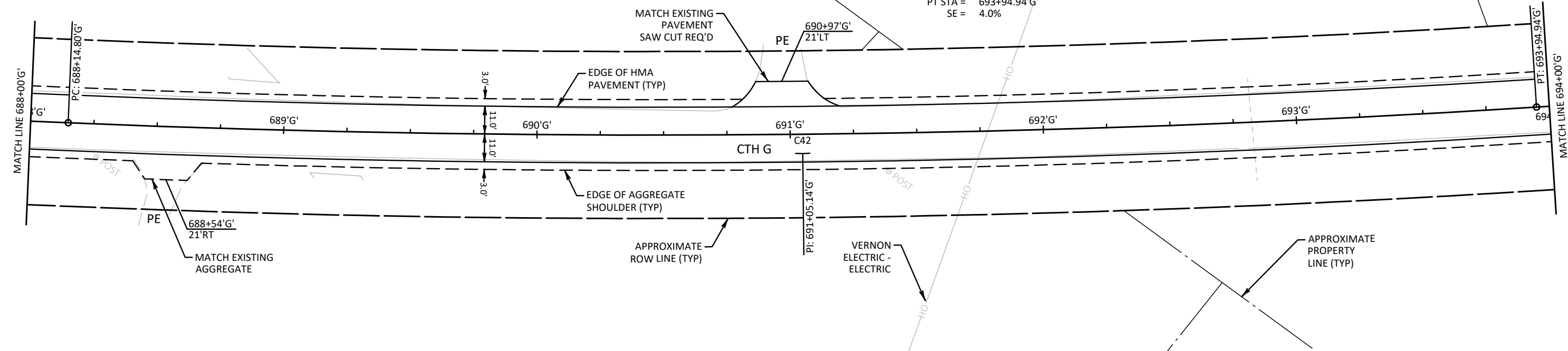


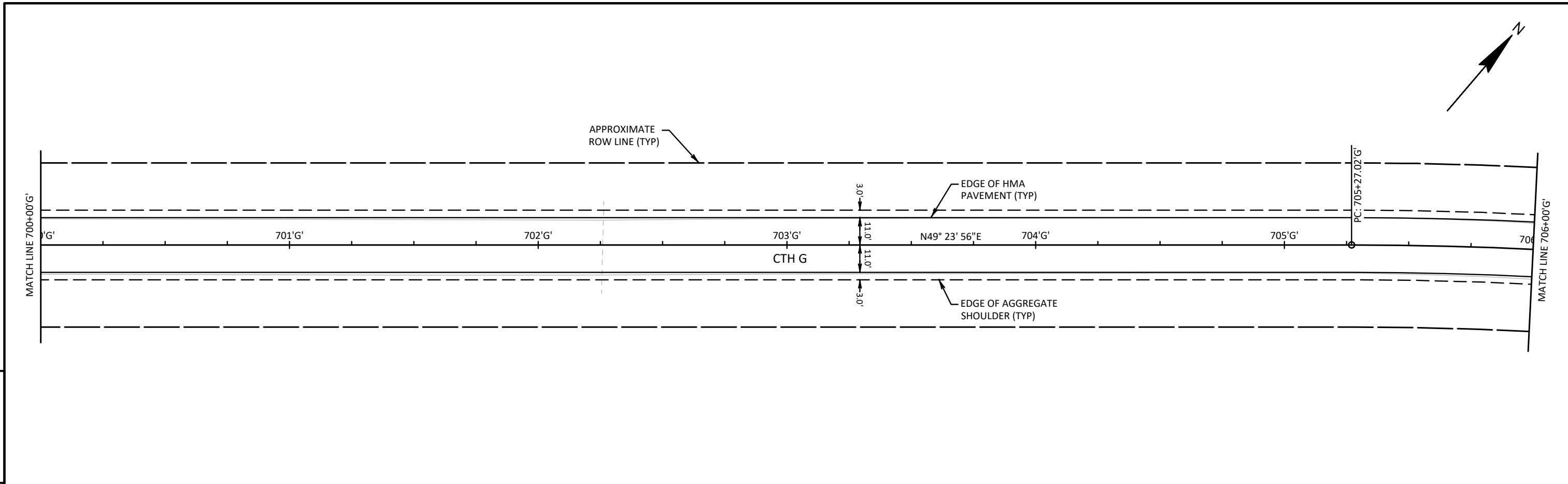
PROJECT NO: 5908-00-75	HWY: CTH G	COUNTY: SAUK	PLAN: CTH G	SHEET	E
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PROJECT NO: 5908-00-75	HWY: CTH G	COUNTY: SAUK	PLAN: CTH G	SHEET	E
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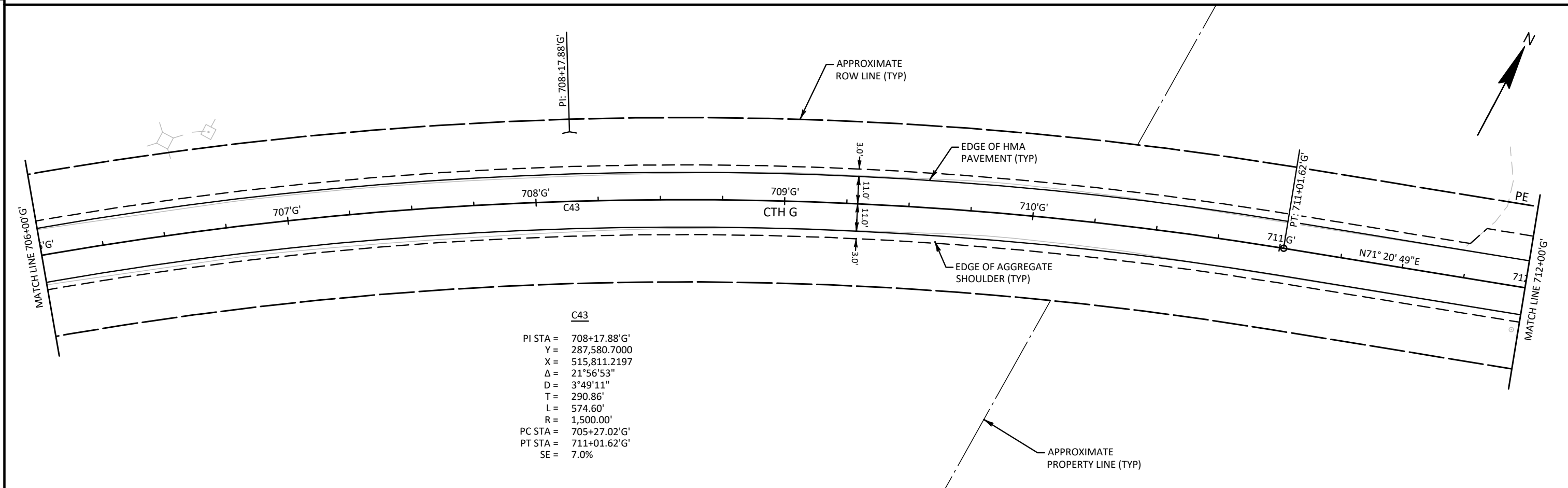
C42  
 PI STA = 691+05.14'G'  
 Y = 286,465.7120  
 X = 514,510.3919  
 Δ = 6°02'37"  
 D = 1°02'30"  
 T = 290.34'  
 L = 580.14'  
 R = 5,500.00'  
 PC STA = 688+14.80'G'  
 PT STA = 693+94.94'G'  
 SE = 4.0%





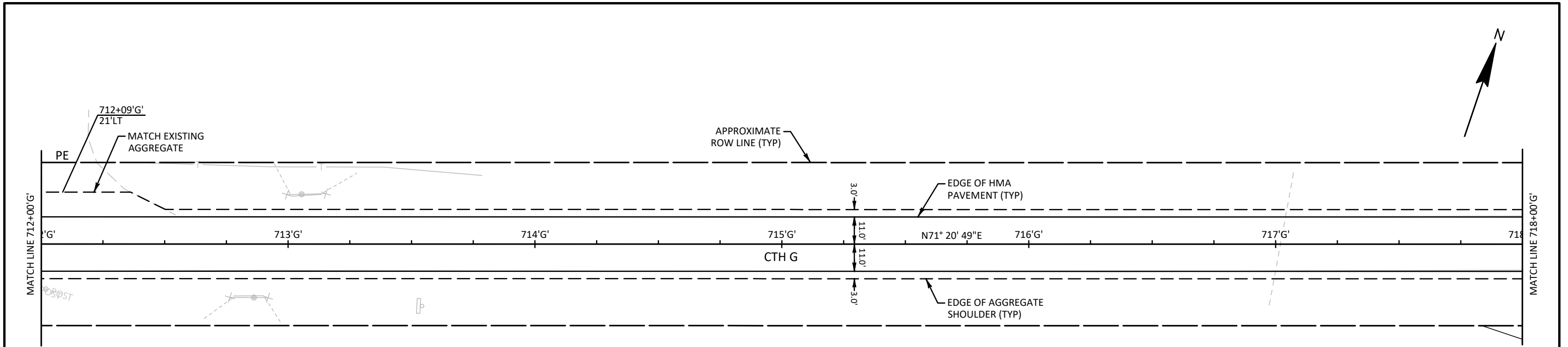
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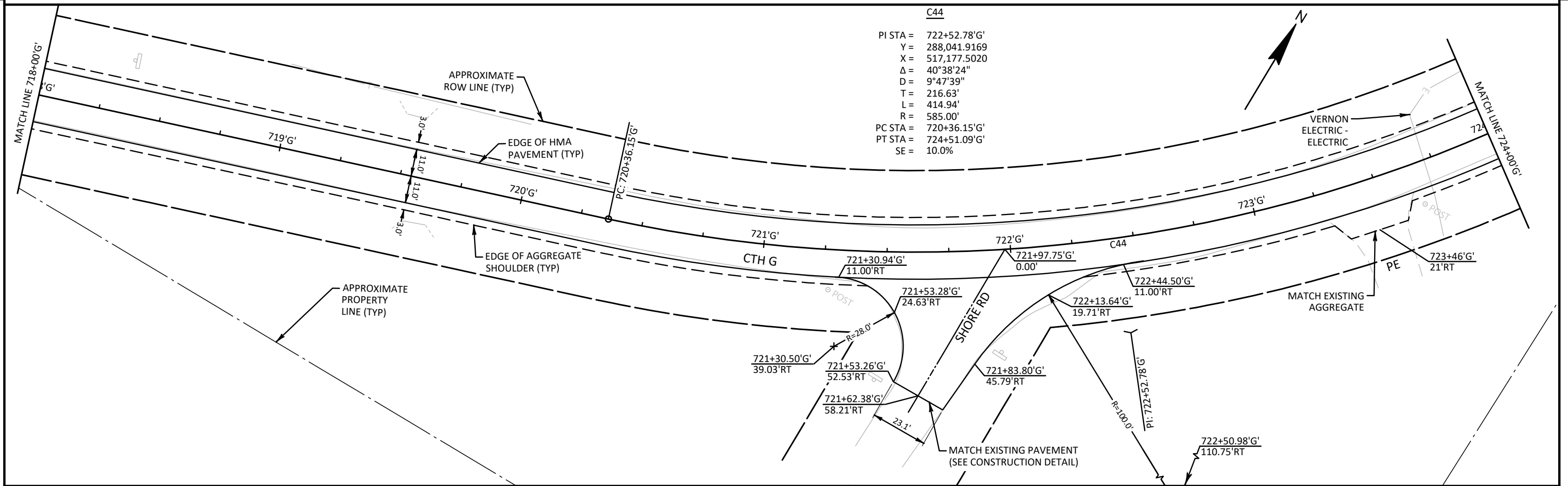
**C43**  
 PI STA = 708+17.88'G'  
 Y = 287,580.7000  
 X = 515,811.2197  
 Δ = 21°56'53"  
 D = 3°49'11"  
 T = 290.86'  
 L = 574.60'  
 R = 1,500.00'  
 PC STA = 705+27.02'G'  
 PT STA = 711+01.62'G'  
 SE = 7.0%



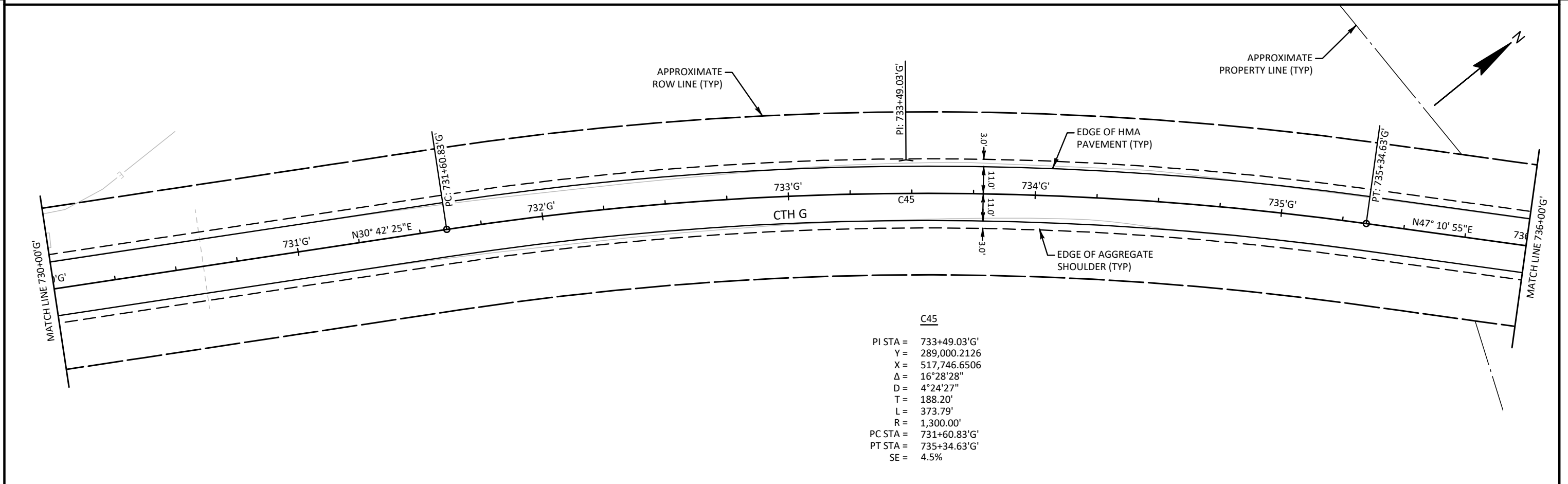
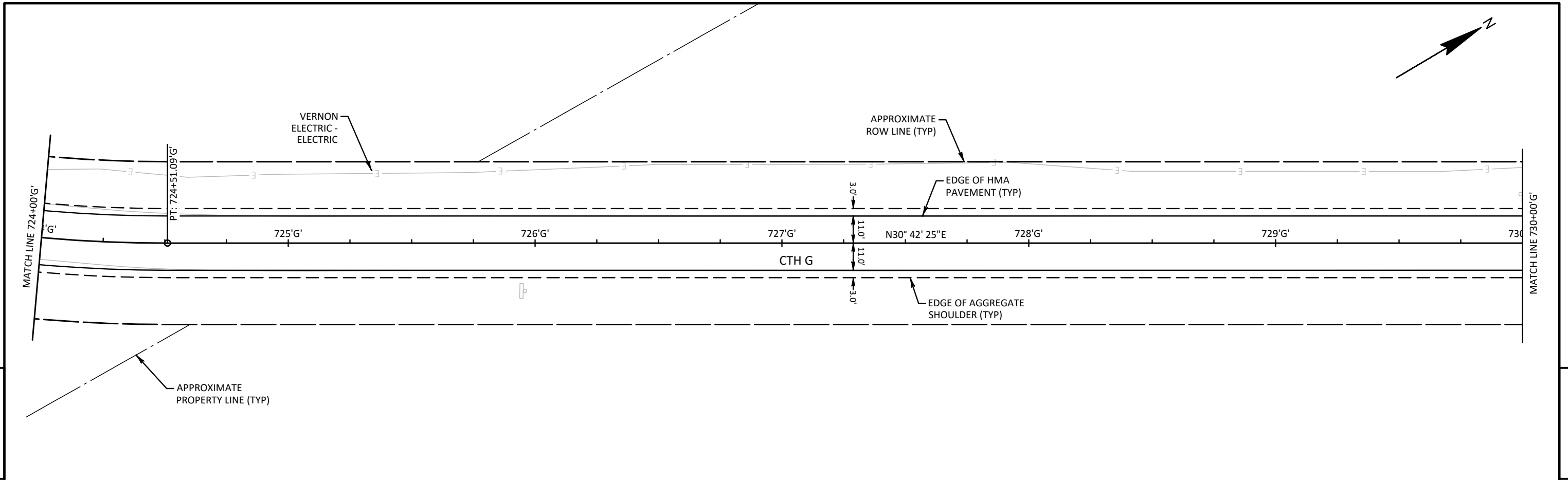


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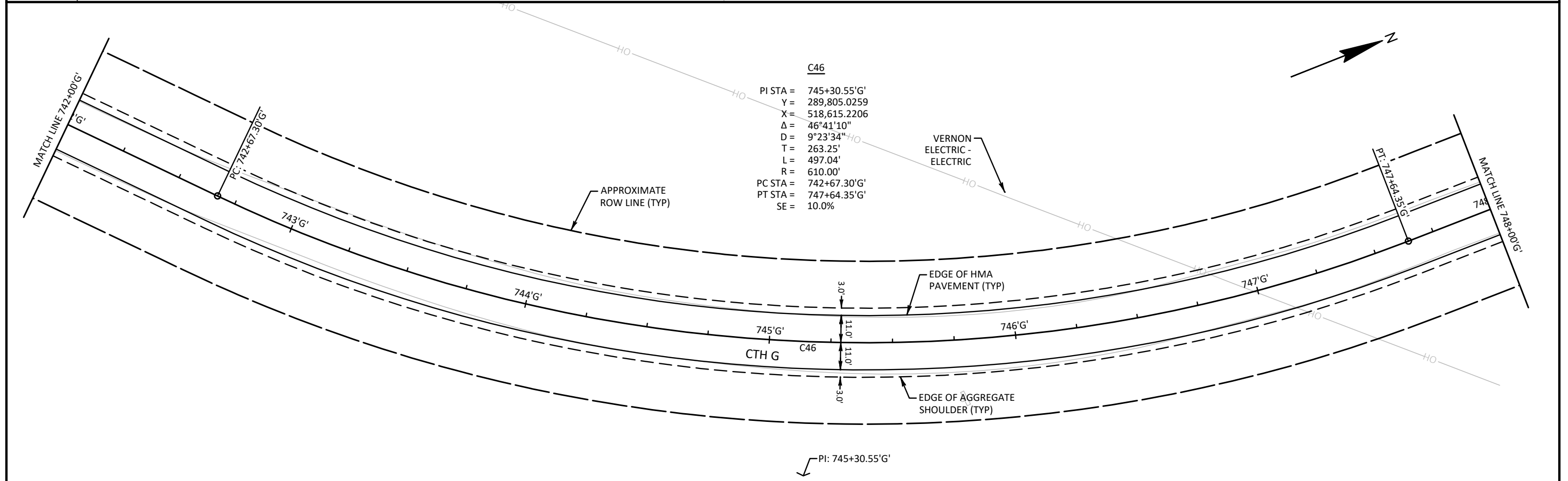
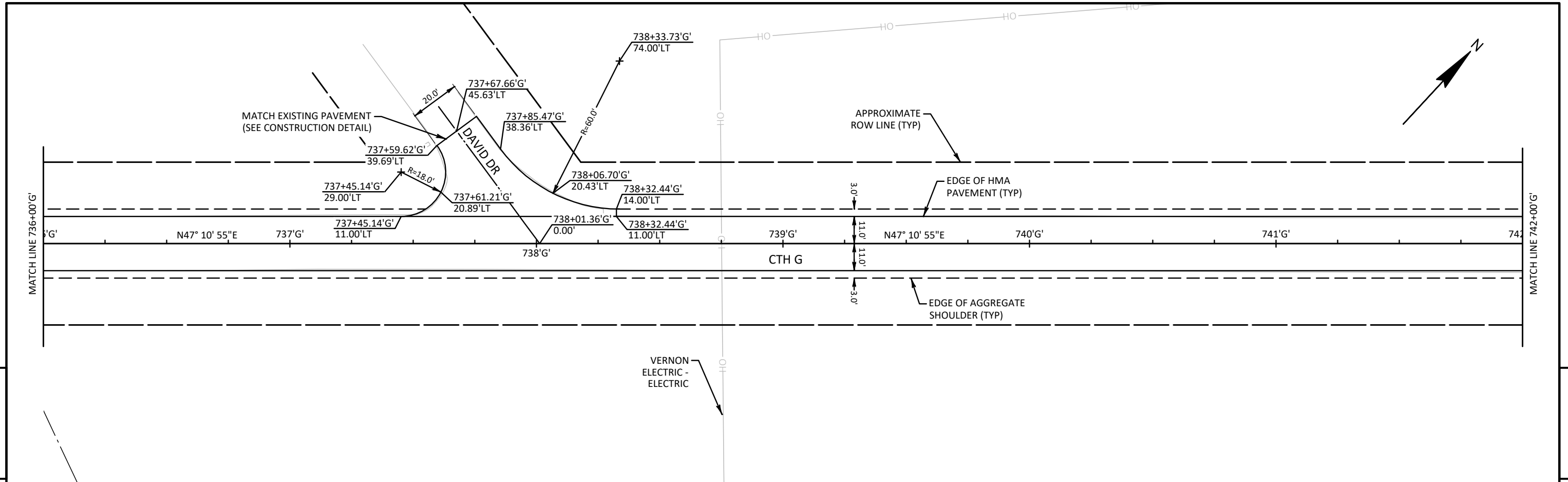
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PROJECT NO: 5908-00-75	HWY: CTH G	COUNTY: SAUK	PLAN: CTH G	SHEET	E
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**C45**  
 PI STA = 733+49.03'G'  
 Y = 289,000.2126  
 X = 517,746.6506  
 Δ = 16°28'28"  
 D = 4°24'27"  
 T = 188.20'  
 L = 373.79'  
 R = 1,300.00'  
 PC STA = 731+60.83'G'  
 PT STA = 735+34.63'G'  
 SE = 4.5%



PROJECT NO: 5908-00-75

HWY: CTH G

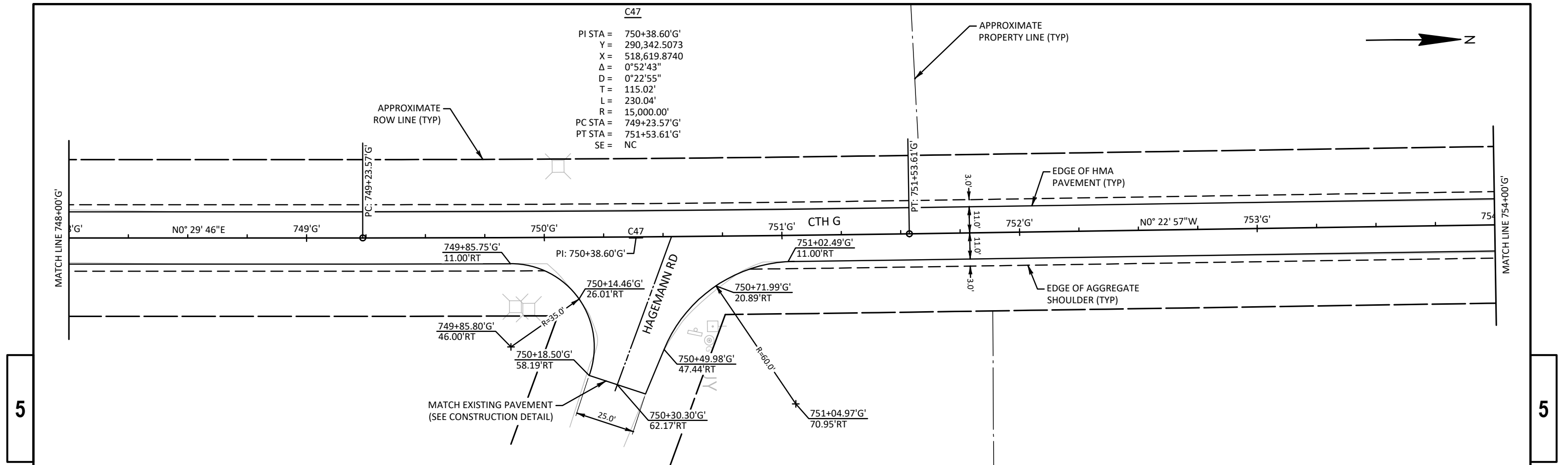
COUNTY: SAUK

PLAN: CTH G

SHEET

E

**C47**  
 PI STA = 750+38.60'G'  
 Y = 290,342.5073  
 X = 518,619.8740  
 Δ = 0°52'43"  
 D = 0°22'55"  
 T = 115.02'  
 L = 230.04'  
 R = 15,000.00'  
 PC STA = 749+23.57'G'  
 PT STA = 751+53.61'G'  
 SE = NC

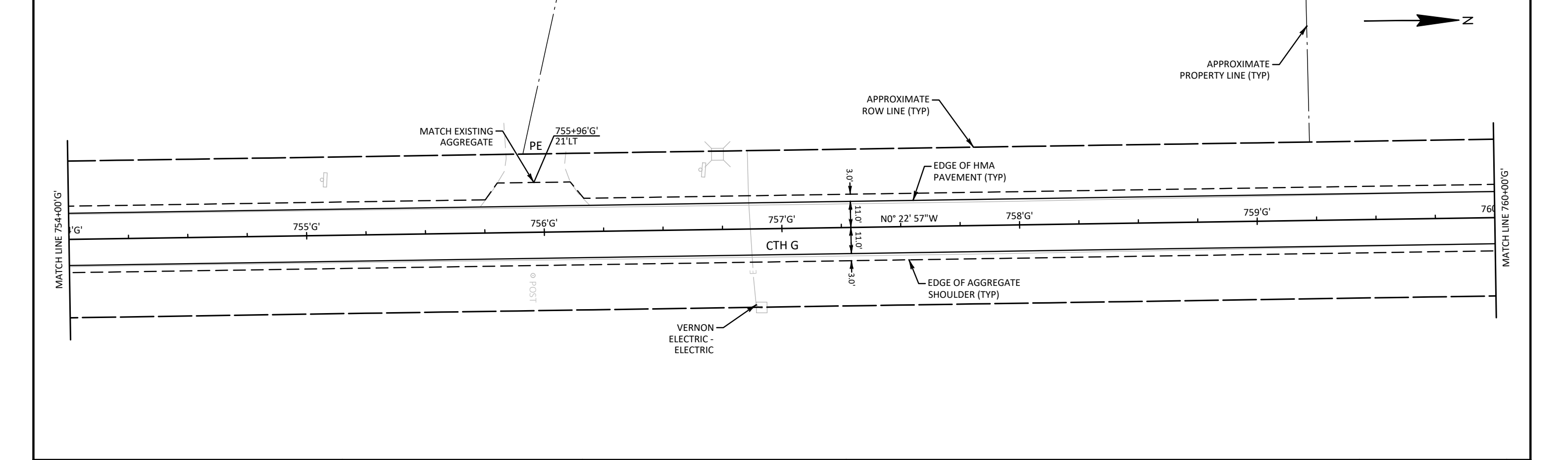


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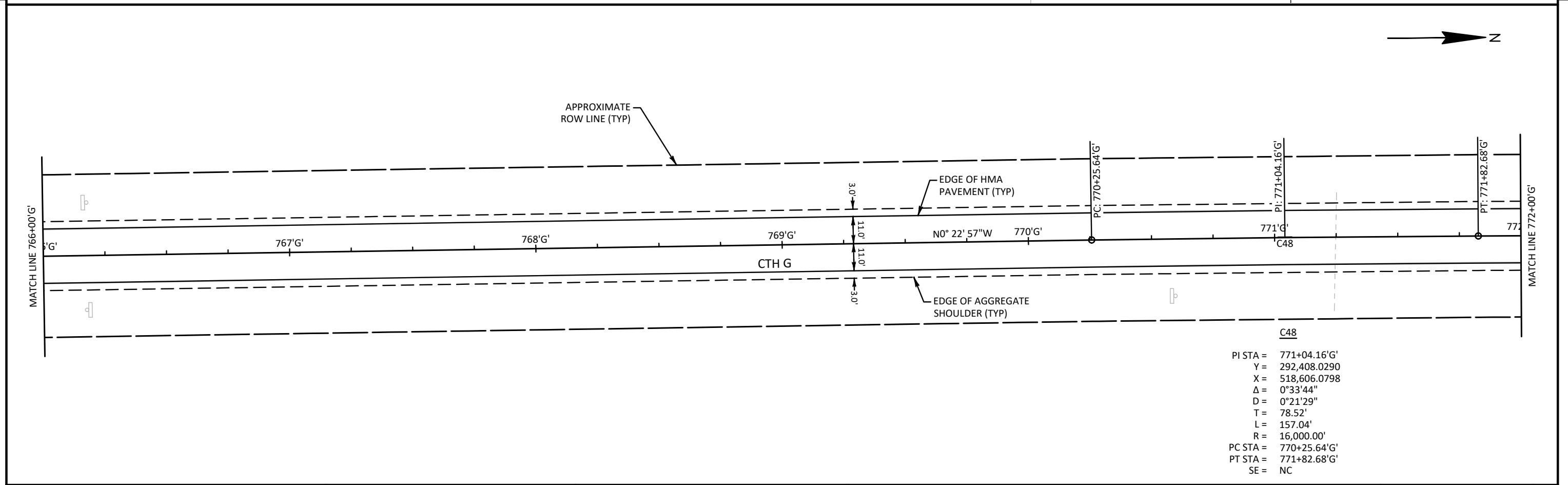
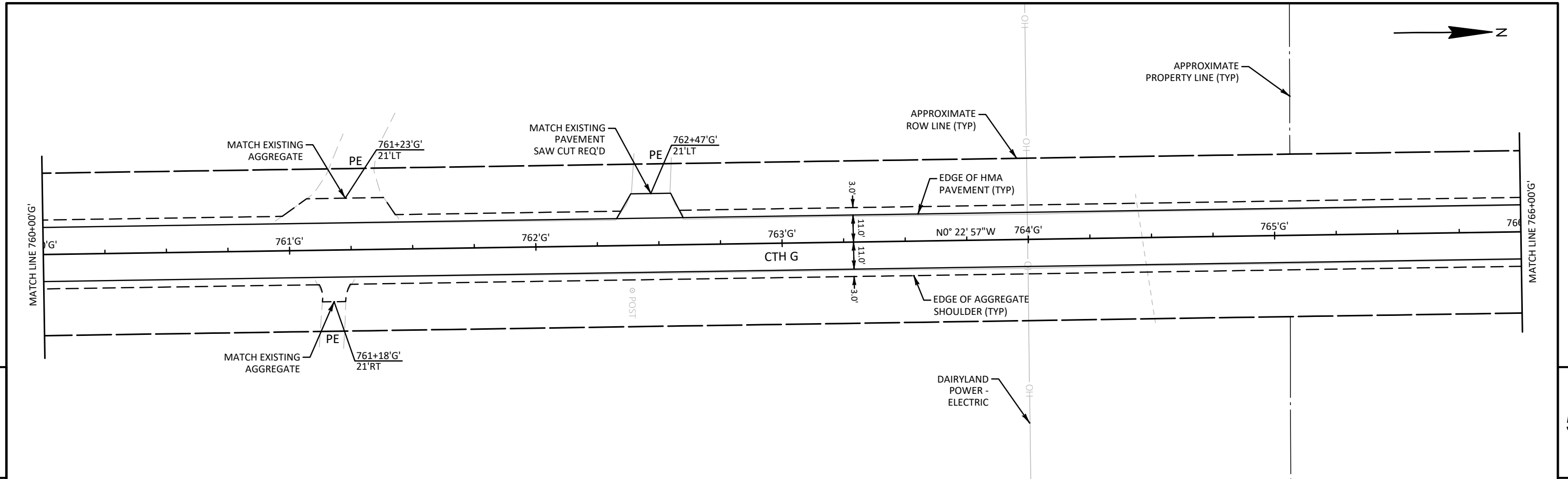
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**PE**  
 755+96'G'  
 21'LT

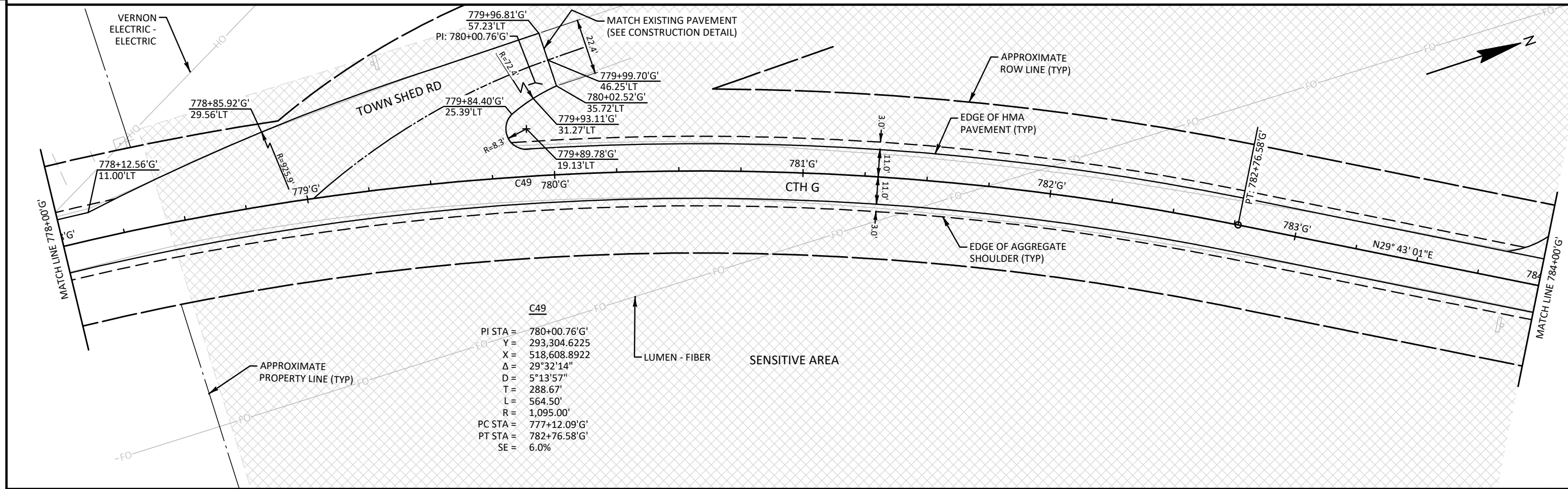
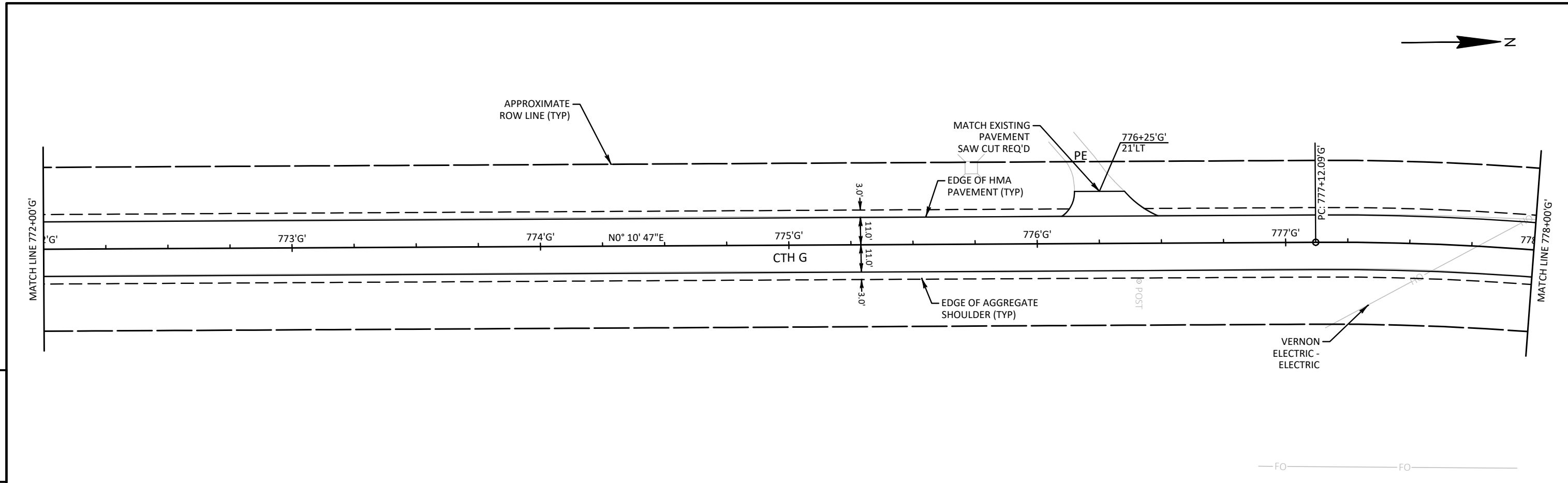
VERNON  
 ELECTRIC -  
 ELECTRIC



PROJECT NO: 5908-00-75	HWY: CTH G	COUNTY: SAUK	PLAN: CTH G	SHEET	E
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C48  
 PI STA = 771+04.16'G'  
 Y = 292,408.0290  
 X = 518,606.0798  
 Δ = 0°33'44"  
 D = 0°21'29"  
 T = 78.52'  
 L = 157.04'  
 R = 16,000.00'  
 PC STA = 770+25.64'G'  
 PT STA = 771+82.68'G'  
 SE = NC



PROJECT NO: 5908-00-75

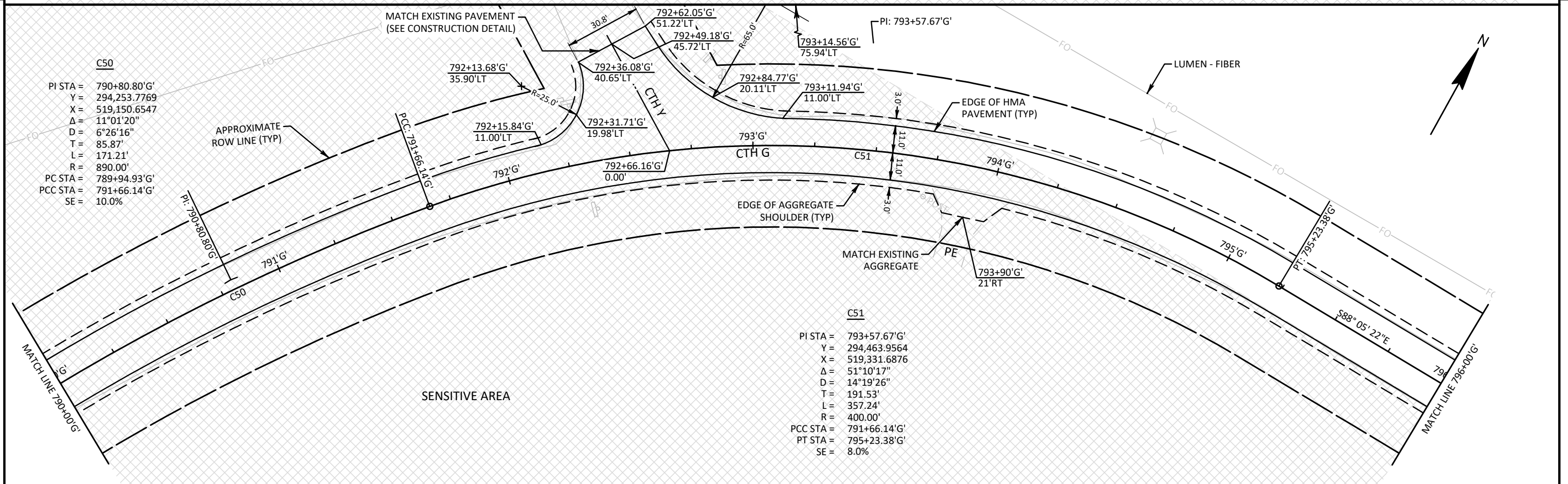
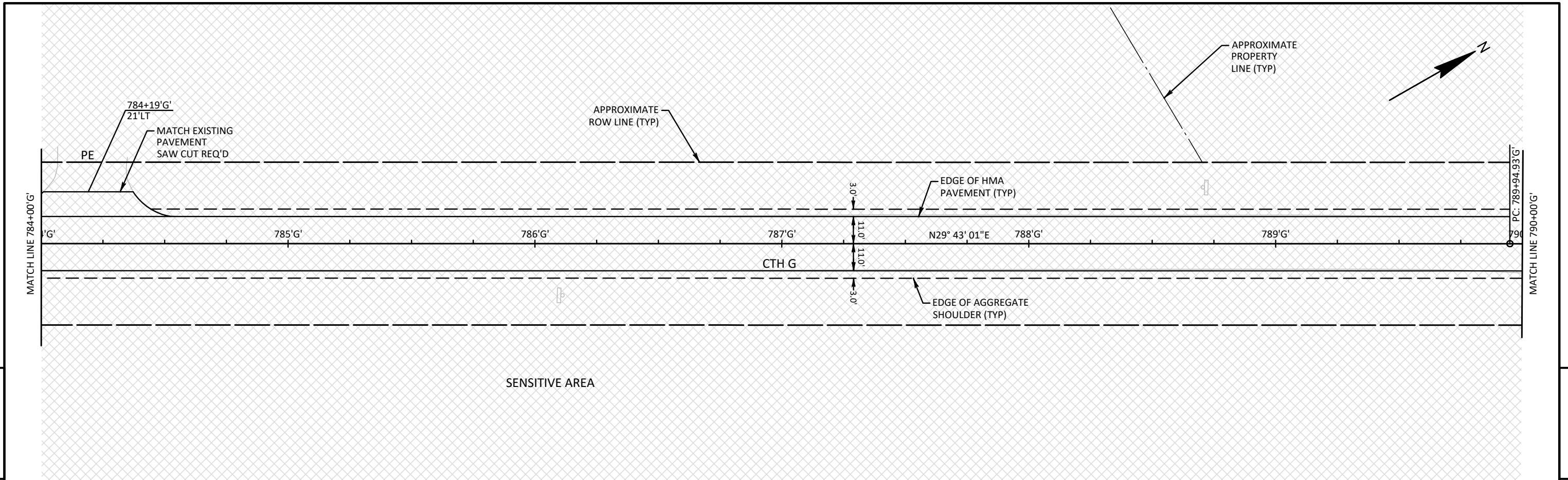
HWY: CTH G

COUNTY: SAUK

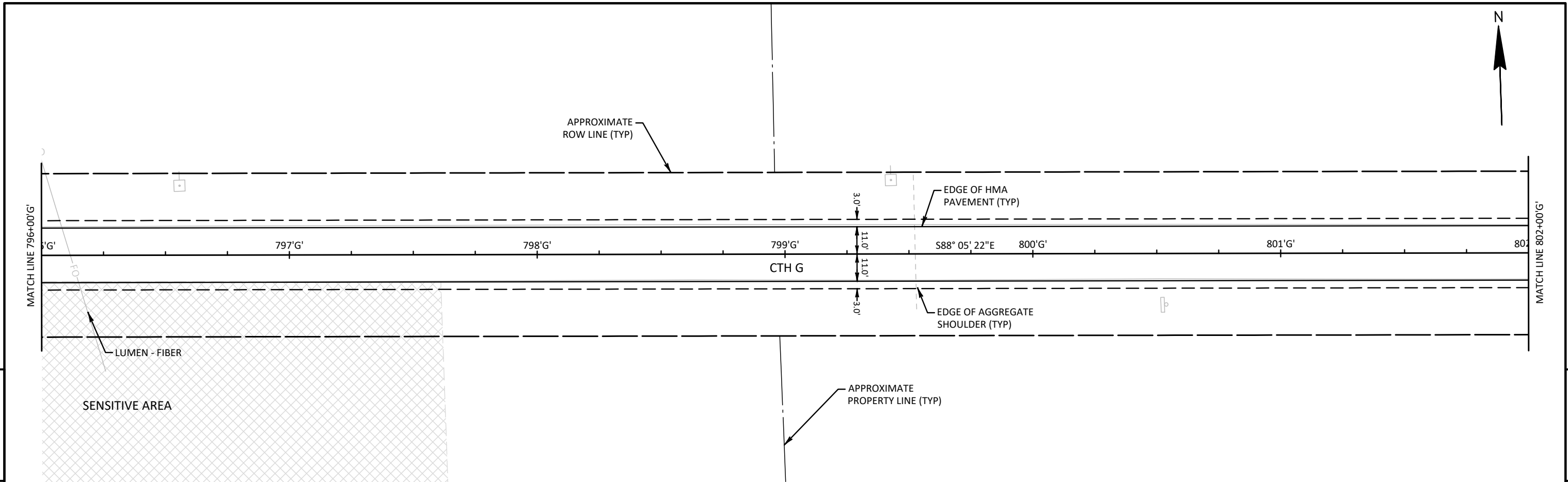
PLAN: CTH G

SHEET

5

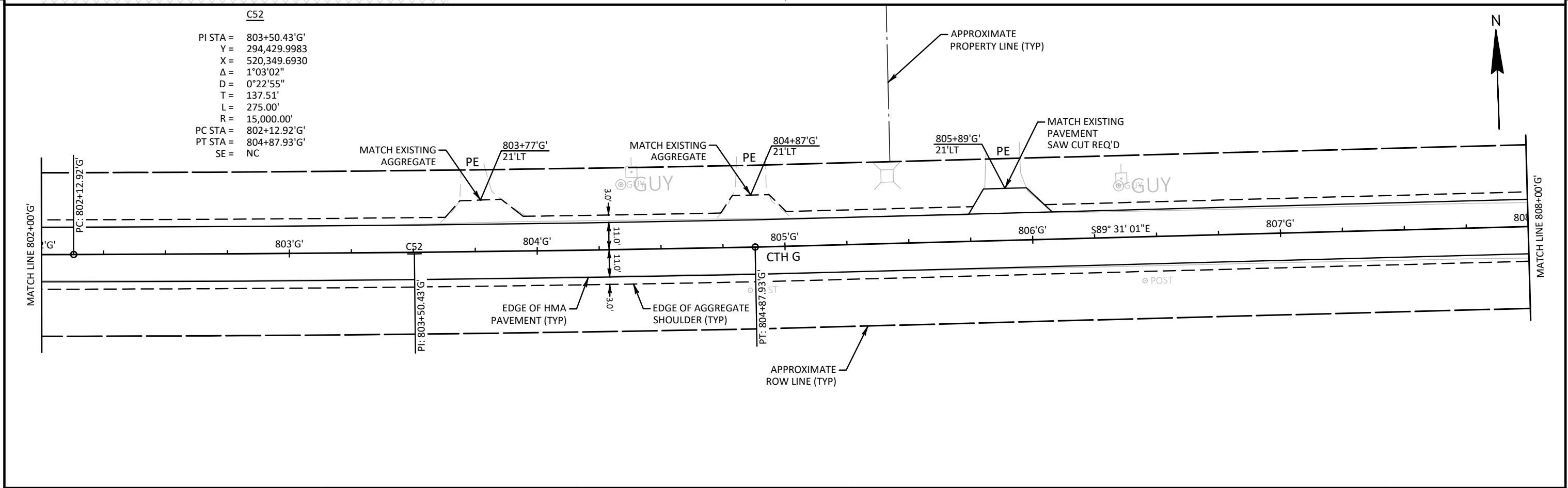


PROJECT NO: 5908-00-75      HWY: CTH G      COUNTY: SAUK      PLAN: CTH G      SHEET: E



5

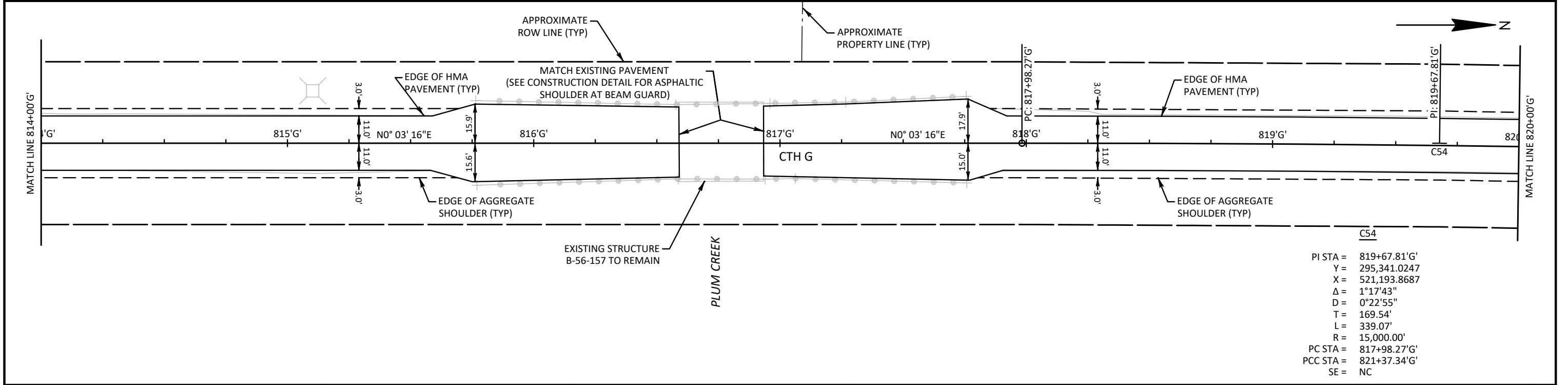
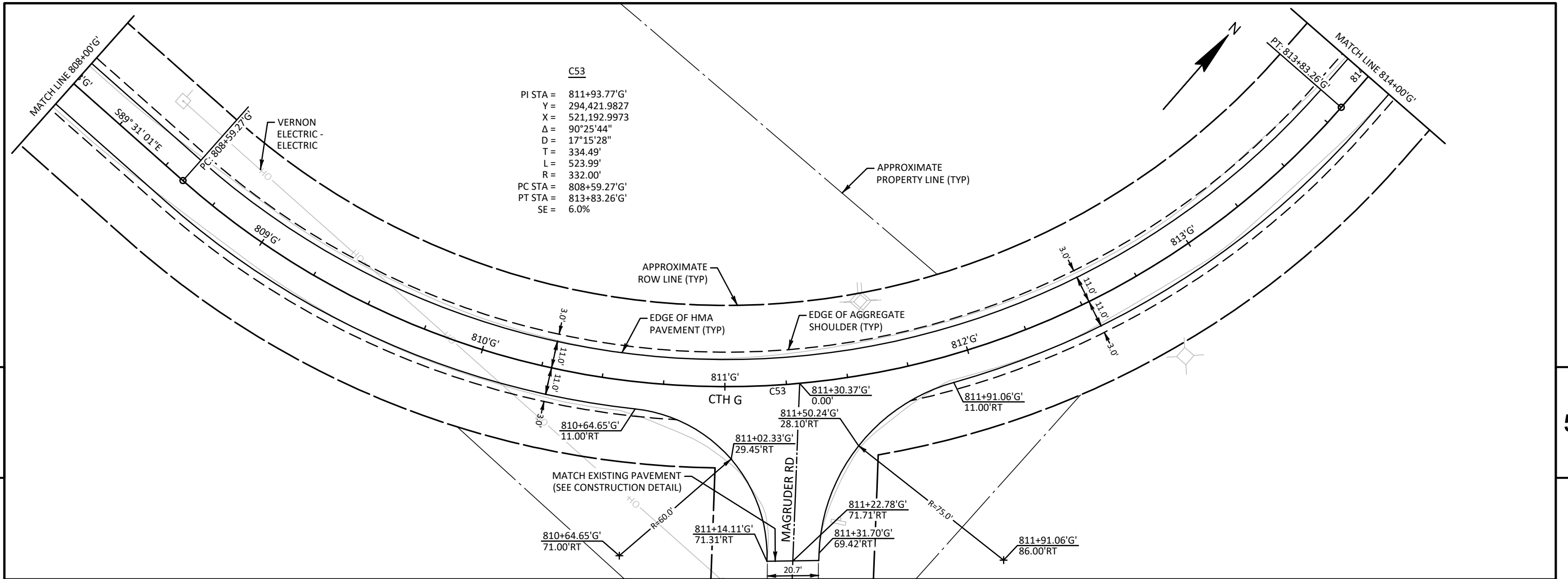
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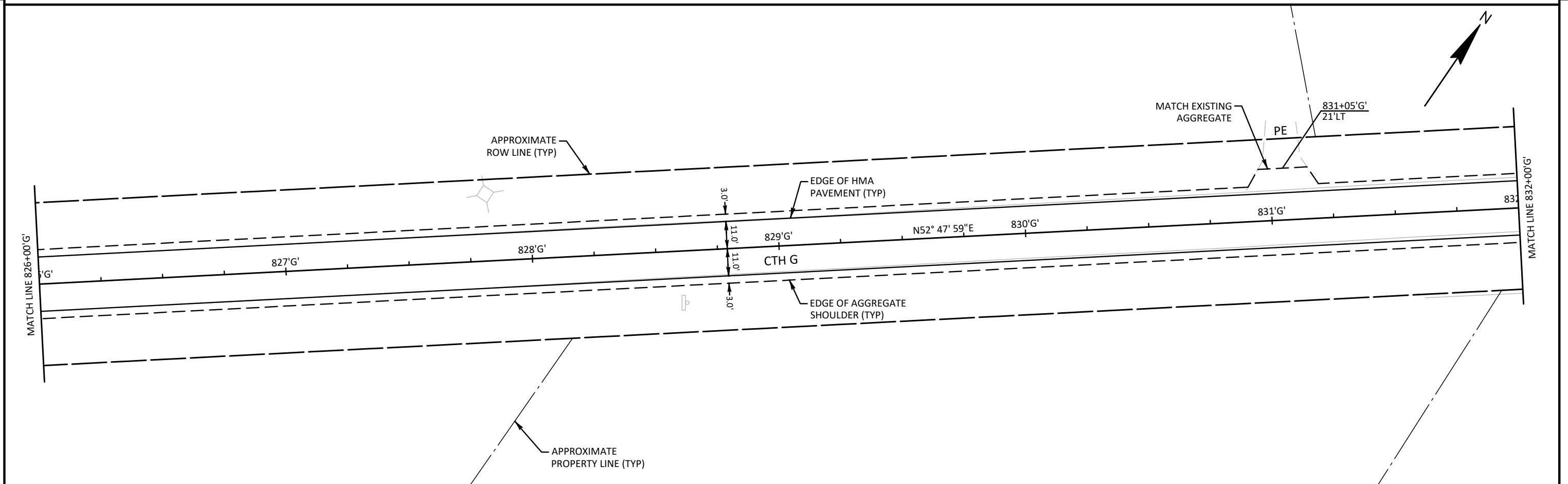
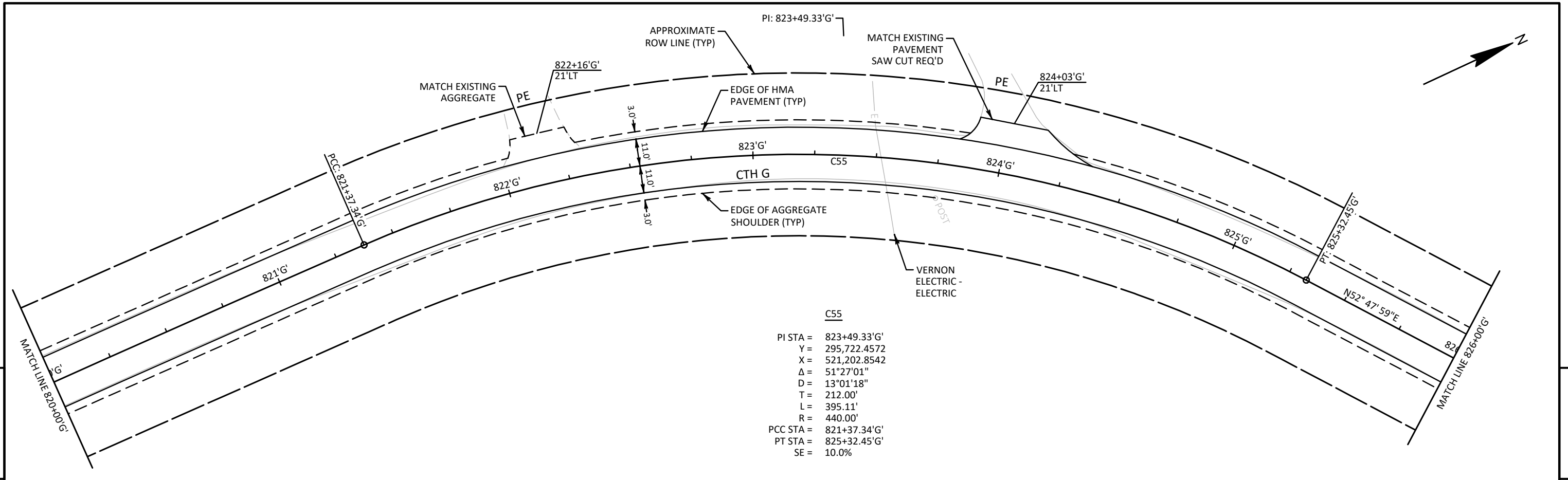


**C52**  
 PI STA = 803+50.43'G'  
 Y = 294,429.9983  
 X = 520,349.6930  
 Δ = 1°03'02"  
 D = 0°22'55"  
 T = 137.51'  
 L = 275.00'  
 R = 15,000.00'  
 PC STA = 802+12.92'G'  
 PT STA = 804+87.93'G'  
 SE = NC

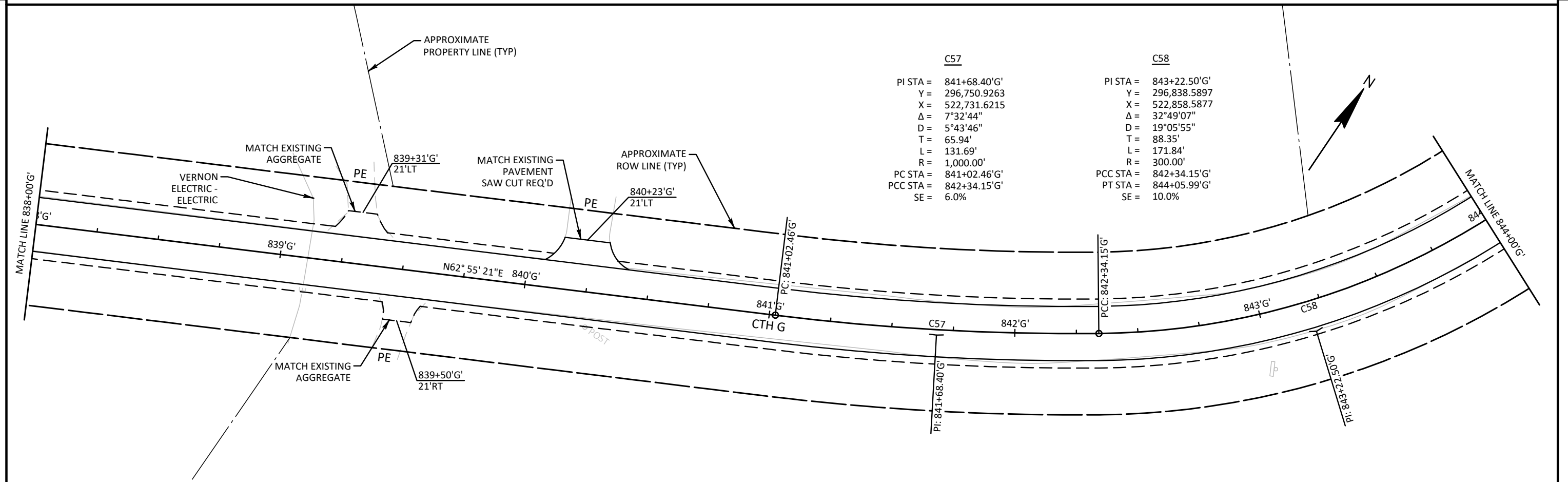
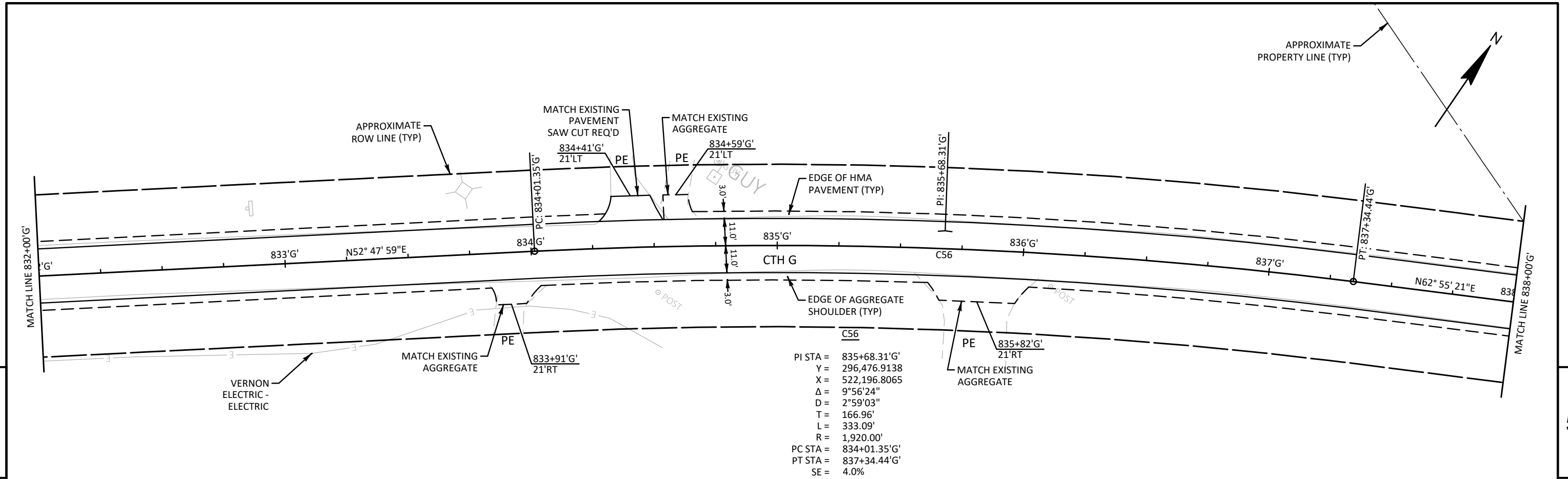
PROJECT NO: 5908-00-75	HWY: CTH G	COUNTY: SAUK	PLAN: CTH G	SHEET	E
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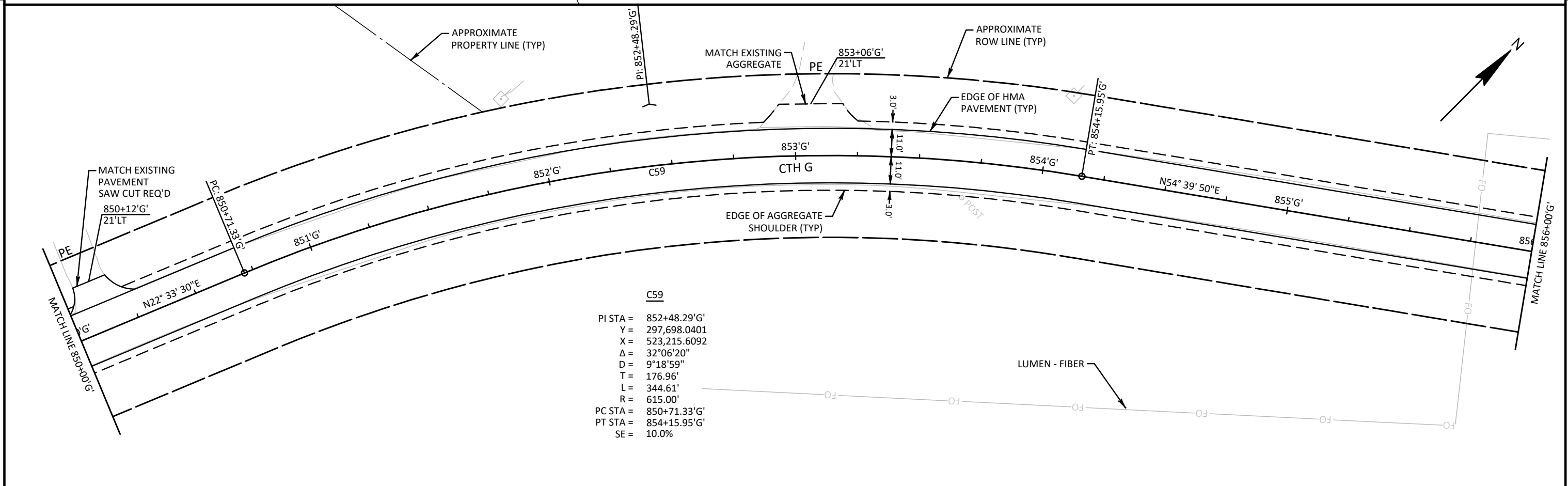
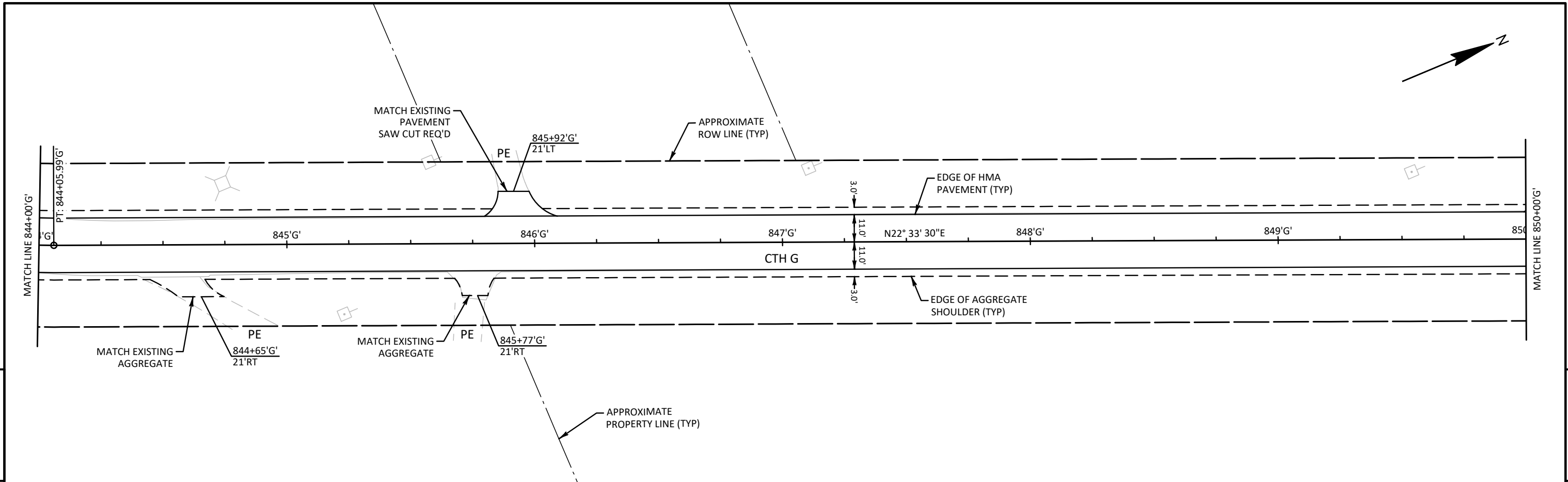




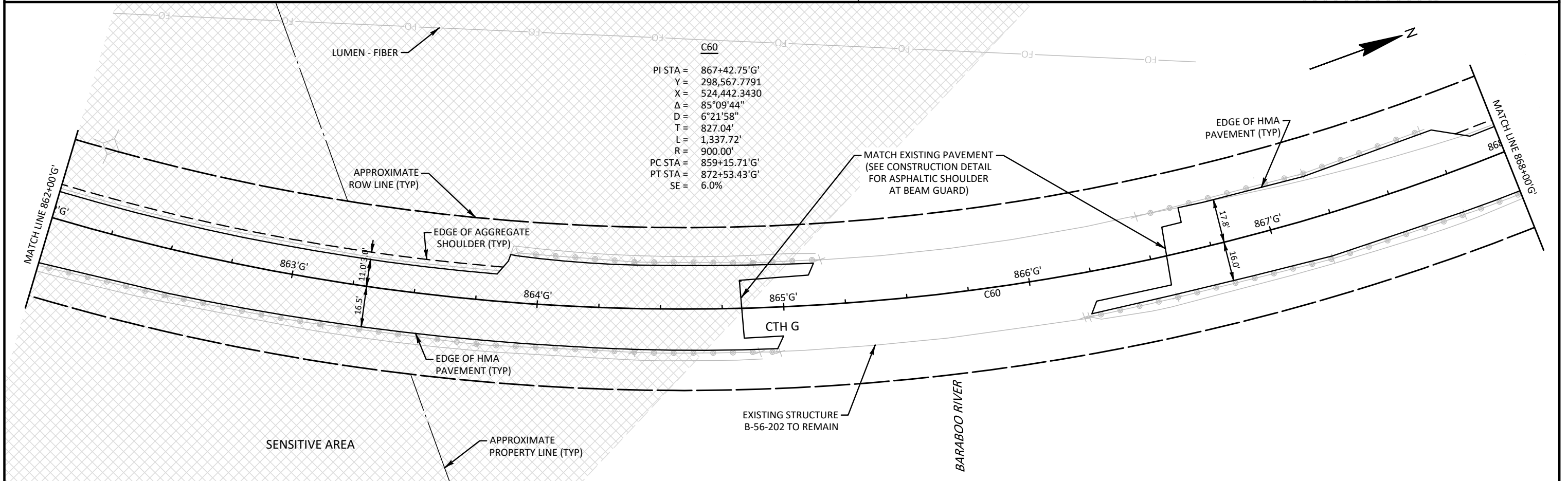
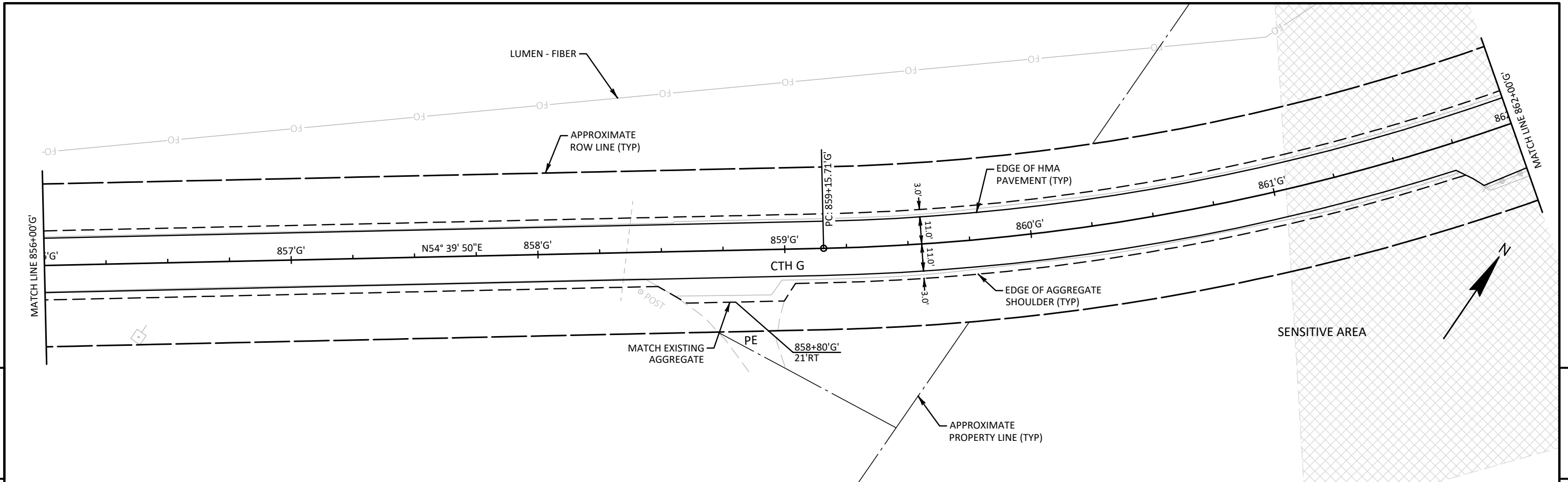
PROJECT NO: 5908-00-75	HWY: CTH G	COUNTY: SAUK	PLAN: CTH G	SHEET	E
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PROJECT NO: 5908-00-75	HWY: CTH G	COUNTY: SAUK	PLAN: CTH G	SHEET	E
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**C59**  
 PI STA = 852+48.29'G'  
 Y = 297,698.0401  
 X = 523,215.6092  
 Δ = 32°06'20"  
 D = 9°18'59"  
 T = 176.96'  
 L = 344.61'  
 R = 615.00'  
 PC STA = 850+71.33'G'  
 PT STA = 854+15.95'G'  
 SE = 10.0%



PROJECT NO: 5908-00-75

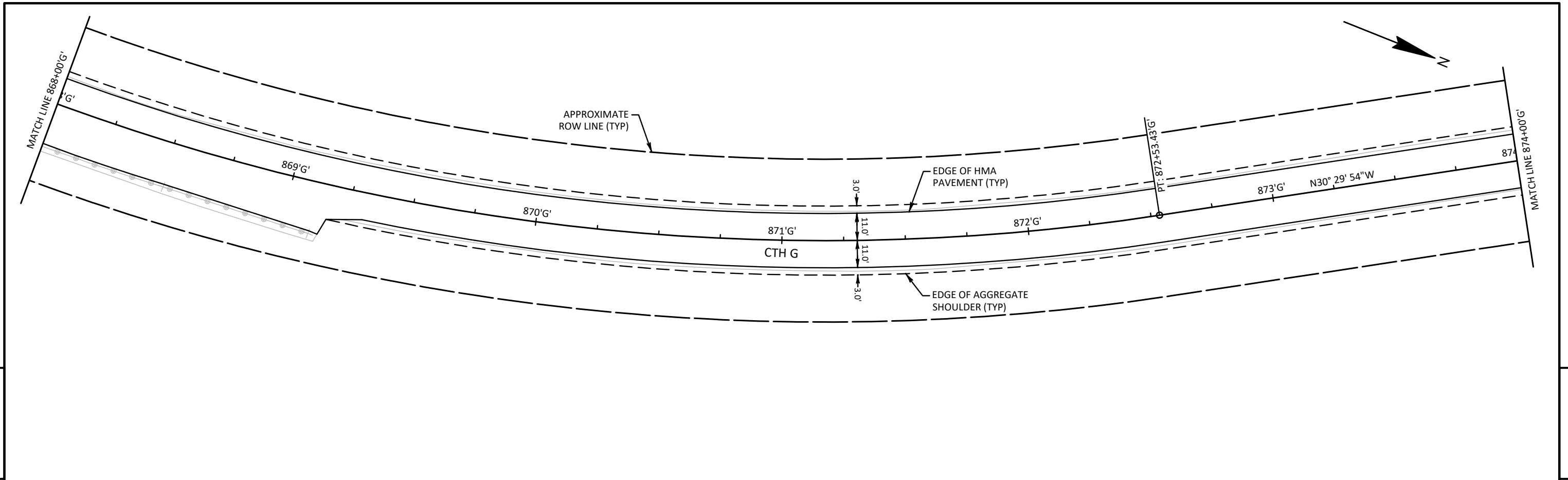
HWY: CTH G

COUNTY: SAUK

PLAN: CTH G

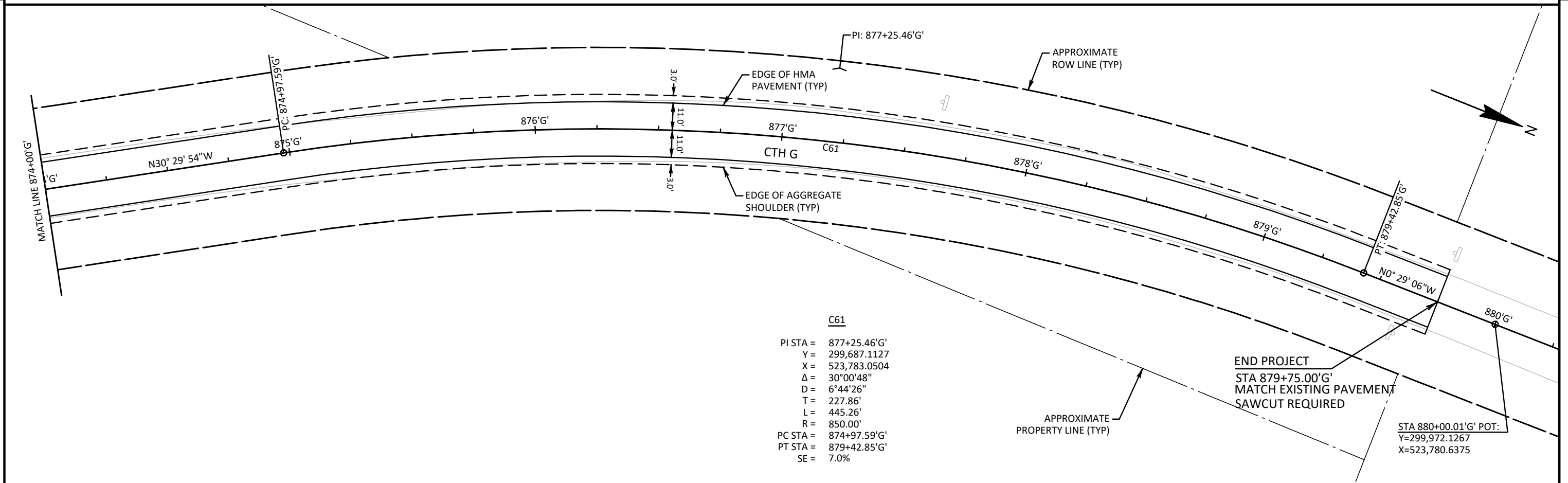
SHEET

E



5

5



**C61**  
 PI STA = 877+25.46'G'  
 Y = 299,687.1127  
 X = 523,783.0504  
 Δ = 30°00'48"  
 D = 6°44'26"  
 T = 227.86'  
 L = 445.26'  
 R = 850.00'  
 PC STA = 874+97.59'G'  
 PT STA = 879+42.85'G'  
 SE = 7.0%

END PROJECT  
 STA 879+75.00'G'  
 MATCH EXISTING PAVEMENT  
 SAWCUT REQUIRED

STA 880+00.01'G' POT:  
 Y=299,972.1267  
 X=523,780.6375

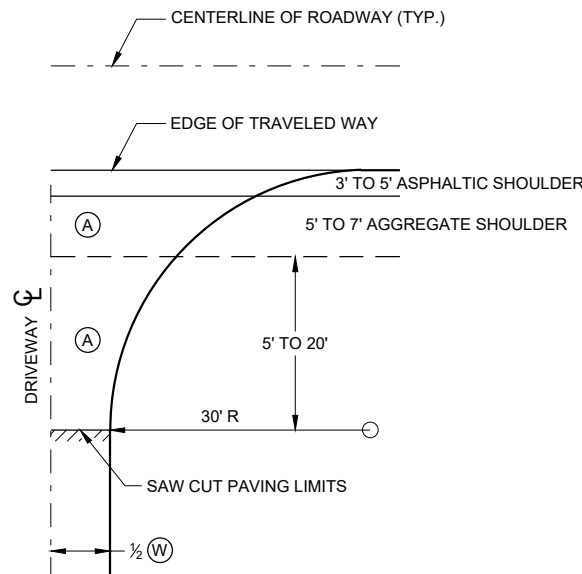
PROJECT NO: 5908-00-75	HWY: CTH G	COUNTY: SAUK	PLAN: CTH G	SHEET	E
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## Standard Detail Drawing List

08D22-01	DRIVEWAYS WITHOUT CURB & GUTTER RESURFACING PROJECTS RURAL
13C19-03	HMA LONGITUDINAL JOINTS
14B15-11A	STEEL PLATE BEAM GUARD, CLASS "A" INSTALLATION & ELEMENTS
14B15-11B	STEEL PLATE BEAM GUARD, CLASS "A" INSTALLATION & ELEMENTS
14B15-11C	STEEL PLATE BEAM GUARD, CLASS "A", INSTALLATION & ELEMENTS
14B29-01	SAFETY EDGE
15C02-08A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-08B	BARRICADES AND SIGNS FOR VARIOUS CLOSURES
15C08-21A	LONGITUDINAL MARKING (MAINLINE)
15C35-05A	PAVEMENT MARKING (INTERSECTIONS)

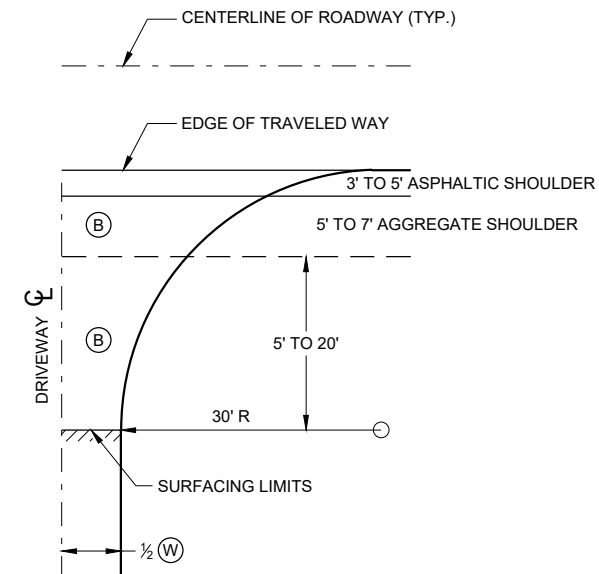
**GENERAL NOTES**

- ① DESIGN WILL DETERMINE FINAL DRIVEWAY ASPHALTIC THICKNESS BASED ON TYPE OF USAGE AND LOADINGS.

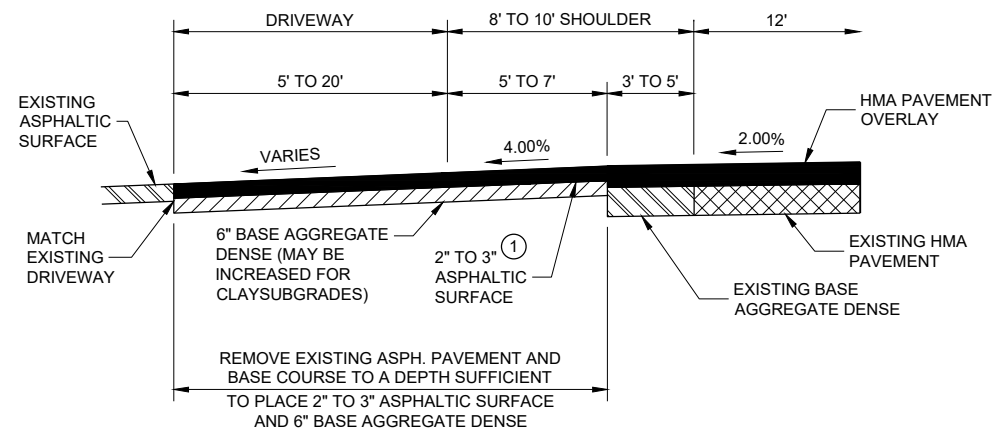


- Ⓐ : PAID FOR AS ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES. (TON)
- Ⓑ : PAID FOR AS BASE AGGREGATE DENSE 1 1/4" (TON)
- ⒲ : DRIVEWAY WIDTH 16' MIN. - 24' MAX.

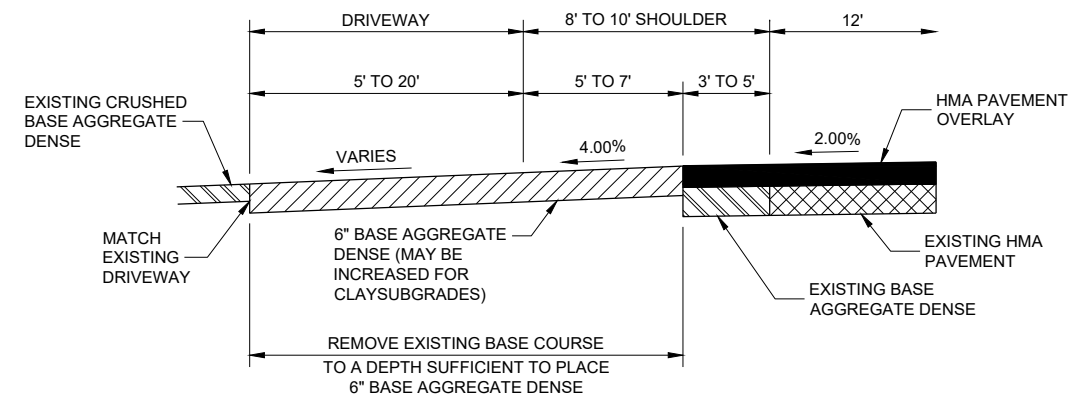
**PLAN VIEW  
HALF SECTION**



**PLAN VIEW  
HALF SECTION**



**PROFILE VIEW  
RURAL ENTRANCE  
WITH ASPHALTIC SURFACE  
RESURFACING PROJECTS**



**PROFILE VIEW  
RURAL ENTRANCE  
WITH AGGREGATE SURFACE  
6" BASE AGGREGATE DENSE  
RESURFACING PROJECTS**

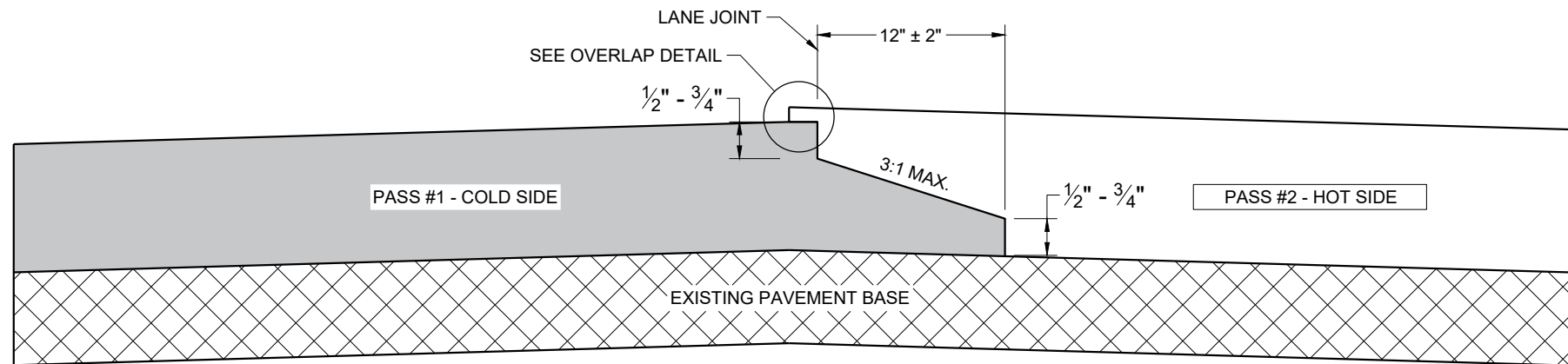
**DRIVEWAYS WITHOUT CURB  
AND GUTTER RESURFACING  
PROJECTS RURAL**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

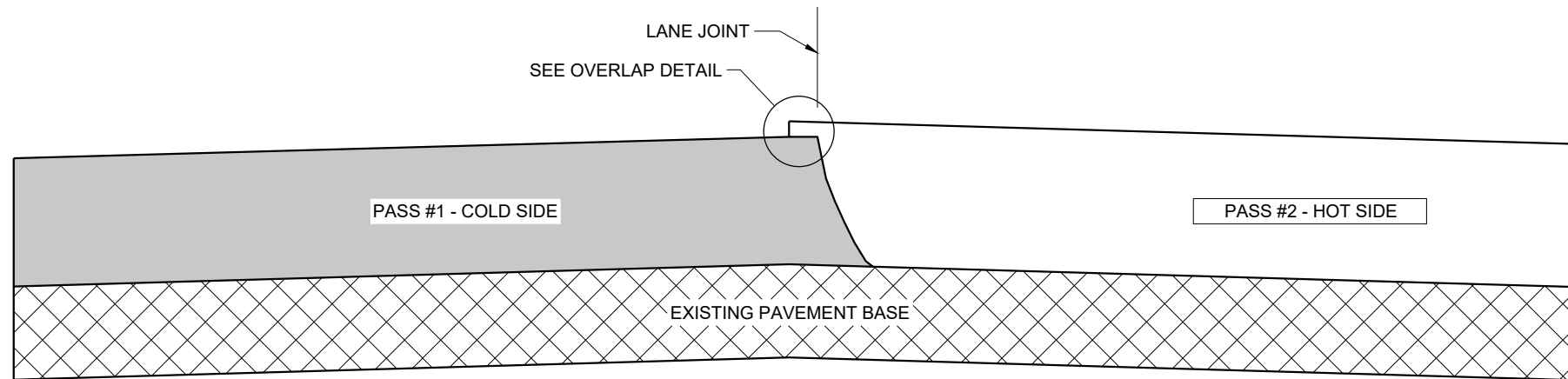
APPROVED  
December 2016 /S/ Rodney Taylor  
DATE ROADWAY STANDARDS DEVELOPMENT  
ENGINEER

FHWA

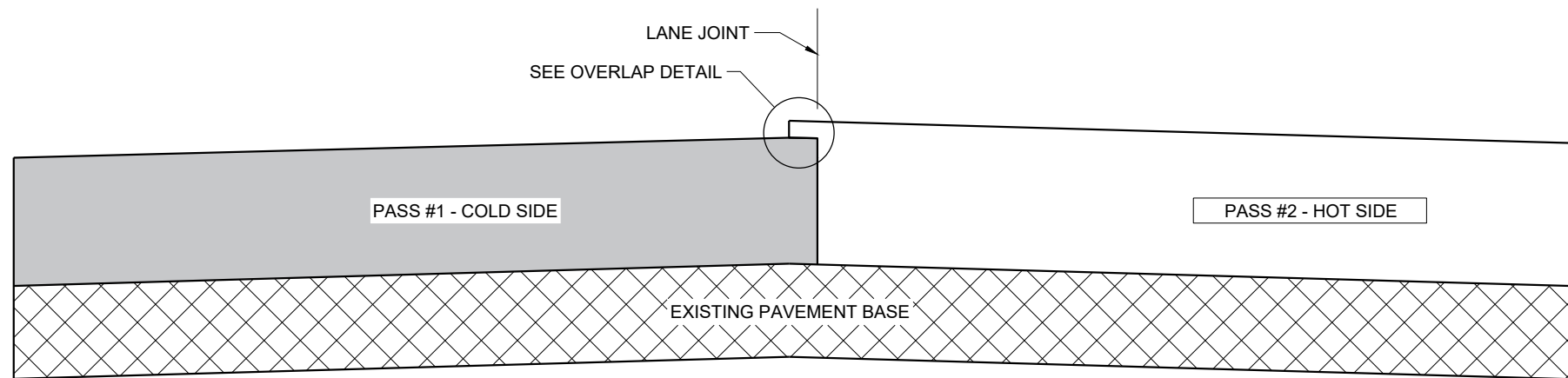




**TYPICAL PAVEMENT CROSS SECTION  
NOTCHED WEDGE JOINT**



**TYPICAL PAVEMENT CROSS SECTION  
VERTICAL JOINT**



**TYPICAL PAVEMENT CROSS SECTION  
VERTICAL JOINT (MILLED)**

**GENERAL NOTES**

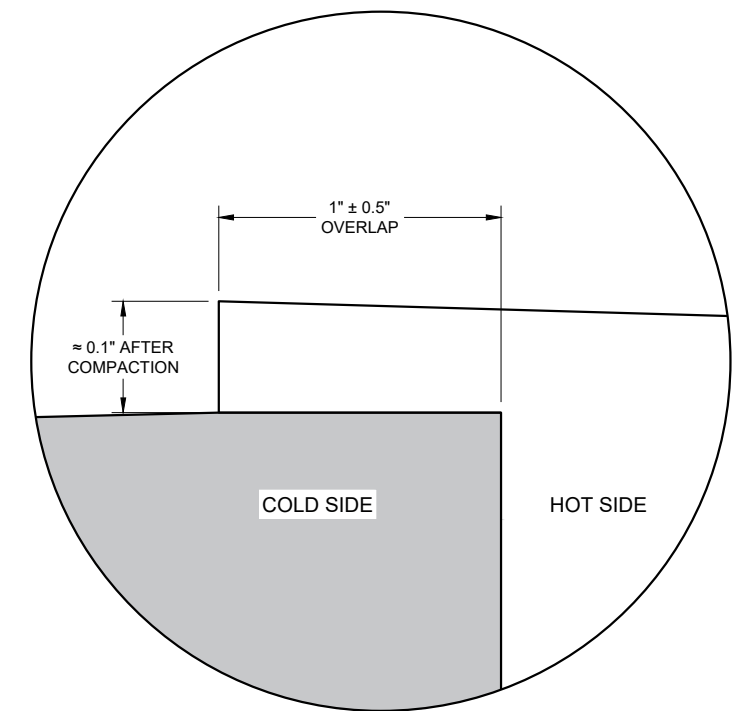
IN ADDITION TO THE DETAILS PROVIDED IN THIS DRAWING, CONFORM TO STANDARD SPECIFICATION 450.3.2.8 FOR WHEN A NOTCHED WEDGE JOINT IS REQUIRED AND FOR GENERAL JOINT CONSTRUCTION REQUIREMENTS.

FOR ALL LONGITUDINAL JOINTS, ENSURE THE PAVER SCREED OVERLAPS THE PREVIOUSLY PLACED PAVEMENT BY  $1" \pm 0.5"$  AND THE HOT SIDE OF THE JOINT REMAINS HIGHER THAN THE COLD SIDE BY APPROXIMATELY 0.1" AFTER FINAL COMPACTION. (IT WILL BE FLUSH WHEN PAVING IN ECHELON.)

ONLY REMOVE THE LONGITUDINAL NOTCHED WEDGE JOINT FOR SMA PAVEMENT OR AS DIRECTED BY THE ENGINEER TO ADDRESS SPECIFIC LENGTHS OF JOINT DAMAGED BY TRAFFIC.

WHEN MILLING BACK OR REMOVING ANY LONGITUDINAL JOINT, LIMIT THE MATERIAL REMOVED TO 2" FROM THE TOP NOTCH OR FROM THE VERTICAL JOINT EDGE ON THE COLD SIDE OF THE JOINT.

USE LONGITUDINAL MILLED JOINT AS PLANS SHOW OR THE AS THE ENGINEER DIRECTS.



**OVERLAP DETAIL (TYPICAL)**

6

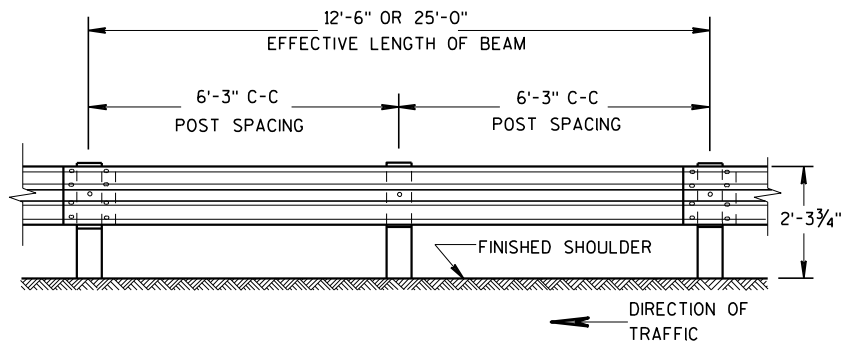
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SDD 13C19 - 03

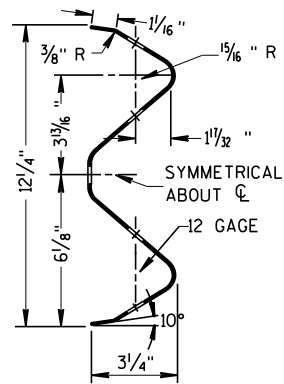
SDD 13C19 - 03

<b>HMA LONGITUDINAL JOINTS</b>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED November 2020 DATE	/S/ Steven Hefel HMA PAVEMENT ENGINEER
FHWA	

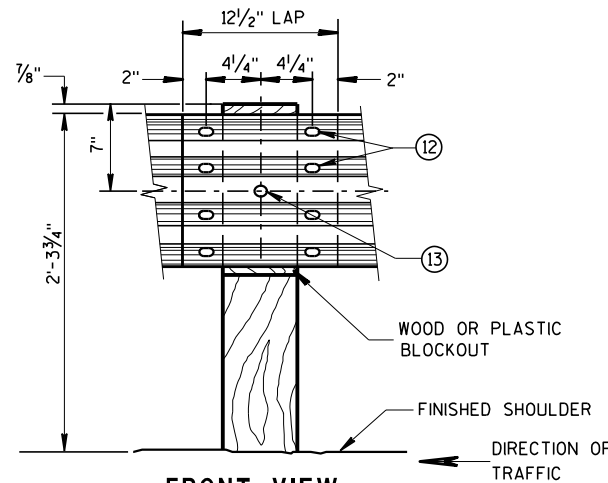




**FRONT VIEW  
POST SPACING STANDARD INSTALLATION**



**SECTION THRU W BEAM**

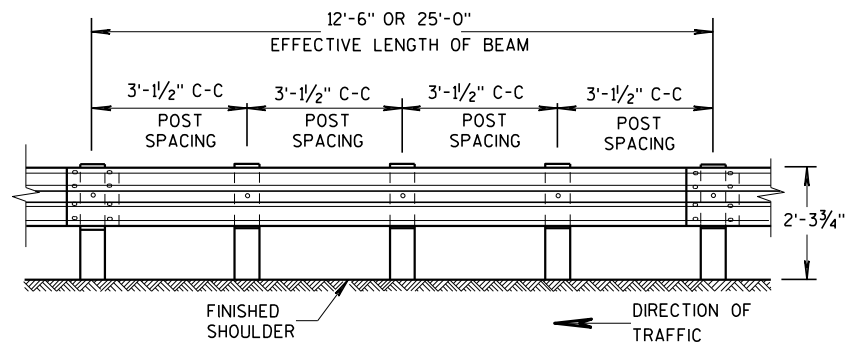


**FRONT VIEW  
BEAM SPLICE AT WOOD POST  
AND POST MOUNTING DETAIL**

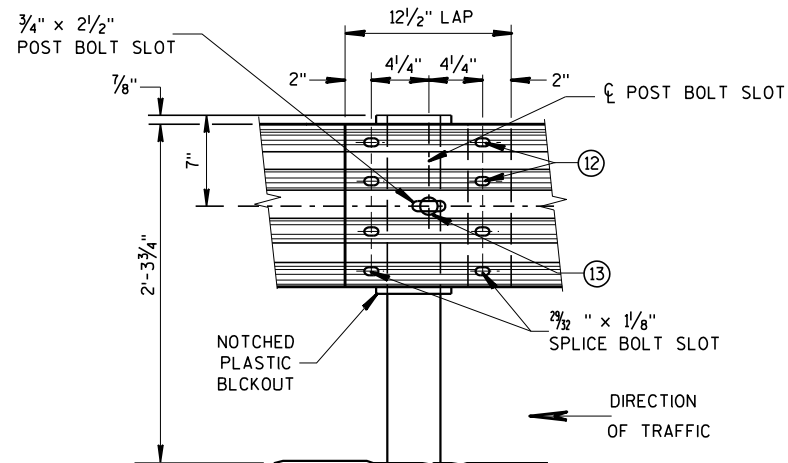
**GENERAL NOTES**

FURNISH GUARDRAIL DEFLECTORS FROM APPROVED PRODUCTS LIST.

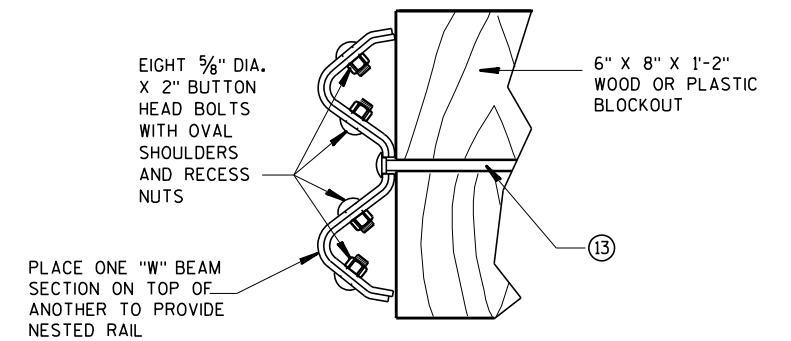
- ⑨ DO NOT INSTALL REFLECTORS ON THE FIRST 50 FEET OF THE APPROACH END OF THE ENERGY ABSORBING TERMINA. START REFLECTORS AT POST #9 AND SPACE EVENLY EVERY 100 FEET (MAX.) TO THE END OF GUARDRAIL RUN, USING A MINIMUM OF 3 REFLECTORS.
- ⑫ 8 - 5/8"  $\phi$  X 2" BUTTON HEAD BOLTS WITH OVAL SHOULDERS & RECESS NUTS.
- ⑬ 5/8" DIA. BUTTON HEAD BOLT AND RECESS NUT WITH 5/8" DIA. F844 FLAT WASHER UNDER NUT.



**FRONT VIEW  
POST SPACING FOR LONGER POST  
AT HALF POST SPACING W BEAM (LHW)**

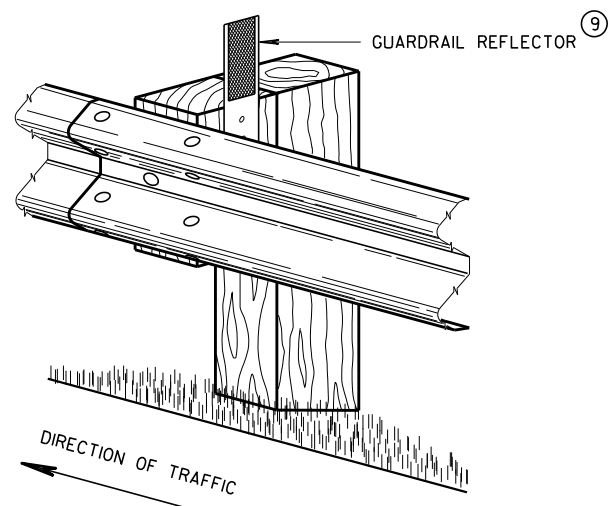


**FRONT VIEW  
BEAM SPLICE AT STEEL POST  
TYPICAL SPlicing DETAILS  
OF STEEL PLATE BEAM GUARD**

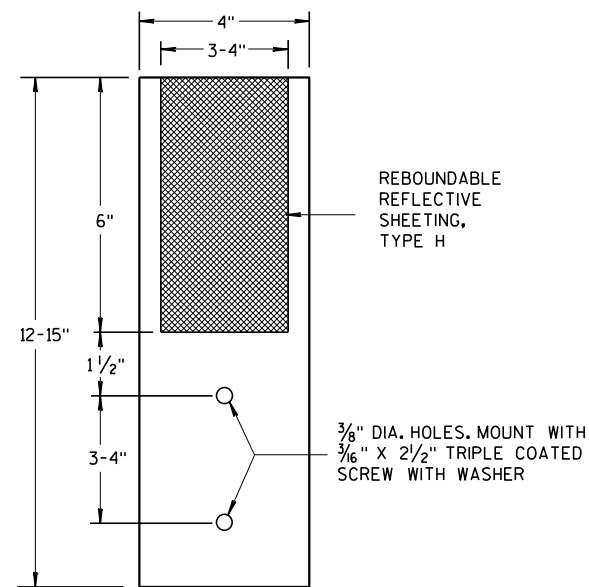


**NESTED W BEAM (NW)**  
USE ALL OTHER STANDARD BEAM GUARD DETAILS FOR  
CONSTRUCTING NESTED W BEAM (NW)

\* USE DOUBLE SIDED WHITE GUARDRAIL REFLECTORS ON ROADWAYS WITH BI-DIRECTIONAL TRAFFIC (NO MEDIAN). USE SINGLE SIDED WHITE (RIGHT SIDE) AND SINGLE SIDED YELLOW (LEFT SIDE) ON ROADWAYS WITH MEDIAN SEPARATION.



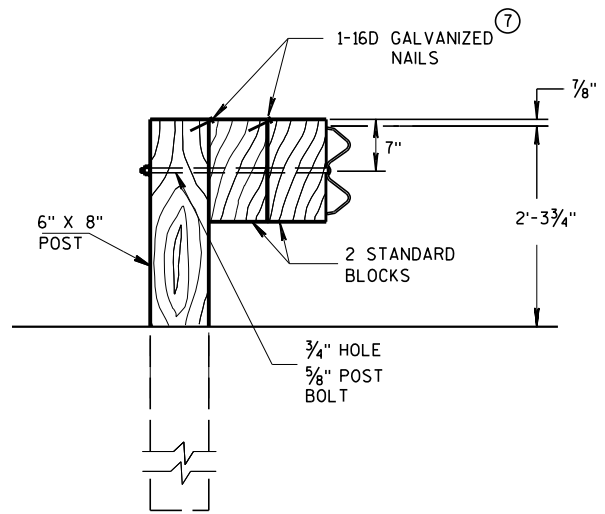
**4" X 12" GUARDRAIL REFLECTOR DETAIL  
AND TYPICAL INSTALLATION \***



**4" x 12" GUARDRAIL REFLECTOR**

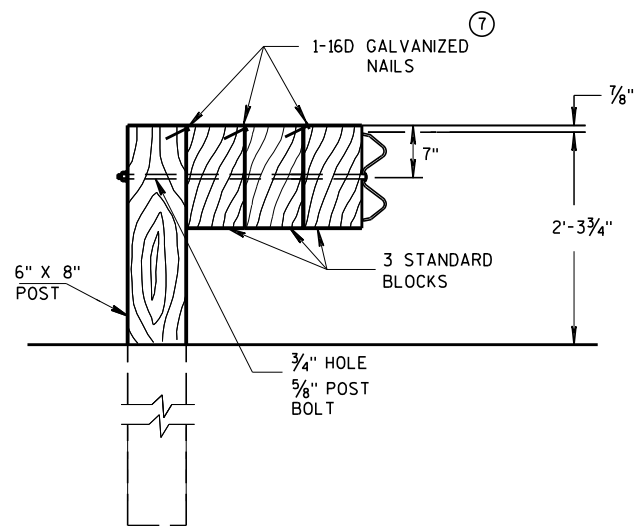
**STEEL PLATE BEAM GUARD,  
CLASS "A",  
INSTALLATION & ELEMENTS**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION



**DETAIL FOR DOUBLE BLOCKS**

THE NUMBER OF DOUBLE BLOCK POSTS WITHIN A BARRIER RUN IS UNLIMITED

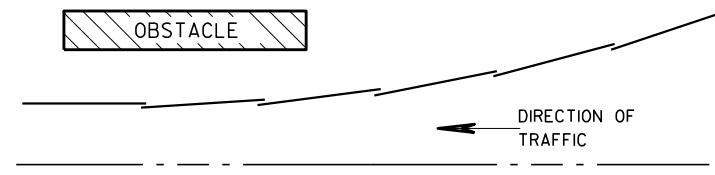


**DETAIL FOR TRIPLE BLOCKS**

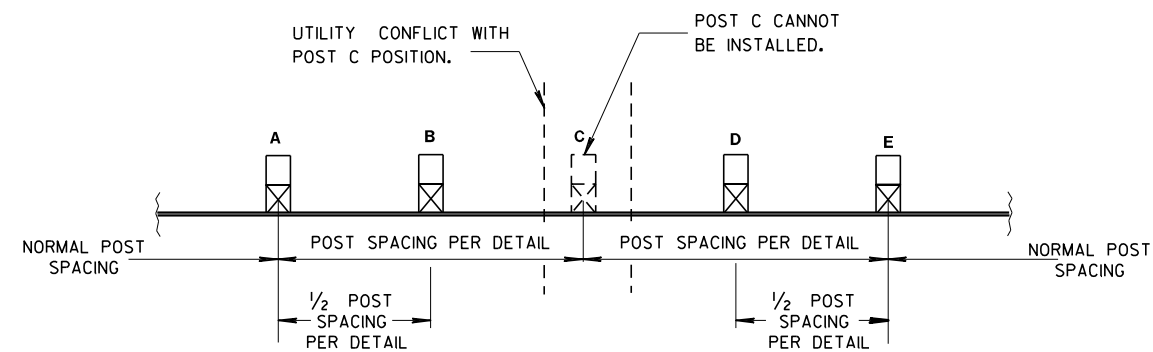
TRIPLE BLOCK DETAIL IS LIMITED TO ONE LOCATION WITHIN A BEAM GUARD RUN.

NOTES: USE DOUBLE OR TRIPLE BLOCKS WHEN UNDERGROUND OBSTACLES PREVENT THE POST FROM BEING INSTALLED.

DO NOT USE EXTRA BLOCKOUTS IF IT CAUSES THE POST TO BE DRIVEN BEYOND SHOULDER HINGE POINT OR CAUSES A FIXED OBJECT TO BE WITHIN THE DEFLECTION DISTANCE OF THE BARRIER.

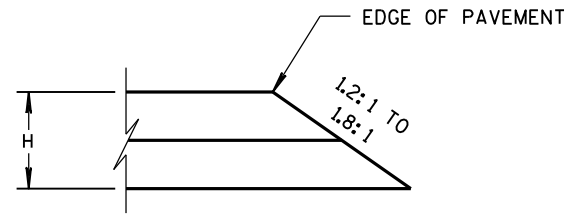


**PLAN VIEW  
BEAM LAPPING DETAIL**

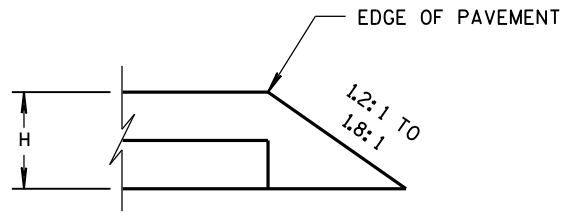


**POST DRIVING FOR CONTINUOUS  
UNDERGROUND OBSTRUCTION**

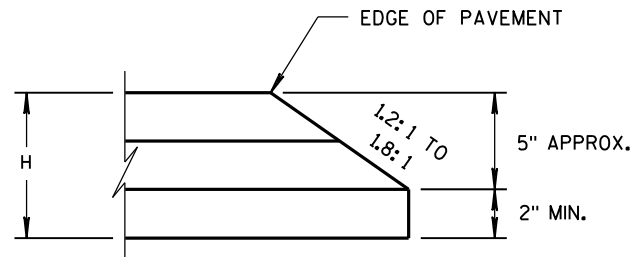
<b>STEEL PLATE BEAM GUARD, CLASS "A", INSTALLATION &amp; ELEMENTS</b>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED June 2017	/s/ Rodney Taylor
DATE	ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR
FHWA	



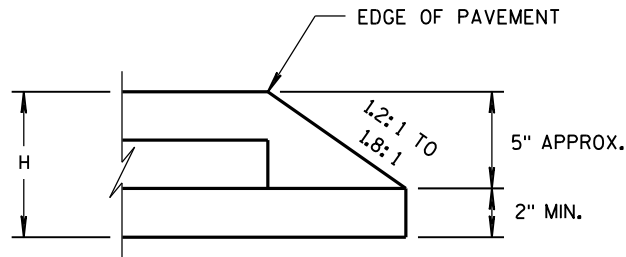
CONSTRUCTED WITH FINAL TWO LAYERS  
FOR H 5" OR LESS



CONSTRUCTED WITH FINAL LAYER  
FOR H 5" OR LESS

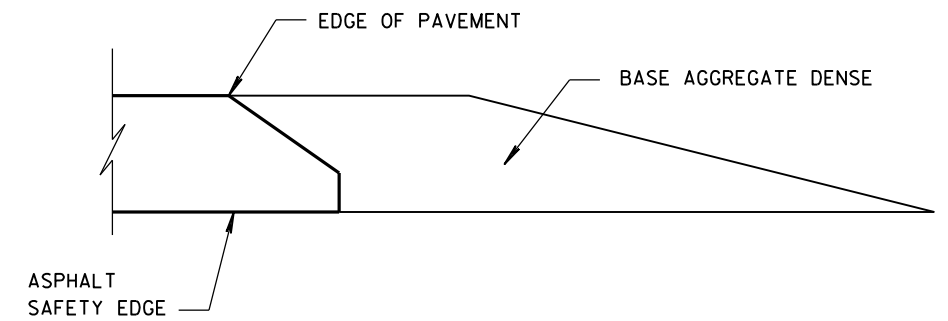


CONSTRUCTED WITH FINAL TWO LAYERS  
FOR H GREATER THAN 5"



CONSTRUCTED WITH FINAL LAYER  
FOR H GREATER THAN 5"

HMA PAVEMENT AND HMA OVERLAYS



FINISHED SHOULDER AGGREGATE PLACEMENT

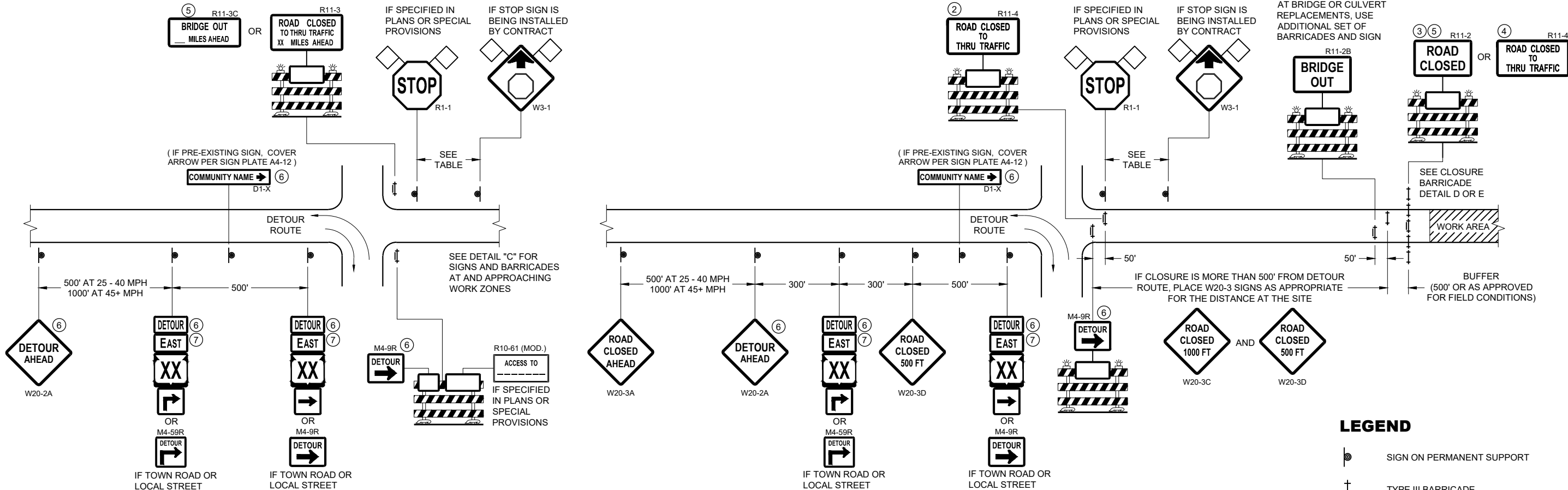
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S.D.D. 14 B 29-1

S.D.D. 14 B 29-1

SAFETY EDGE <sub>SM</sub>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED DATE 11/30/2012	/s/ Jerry H. Zogg ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA	



**DETAIL A  
MAINLINE CLOSURE WITH POSTED DETOUR**

WORK ZONE GREATER THAN OR EQUAL TO 1/2 MILE FROM  
DETOUR ROUTE ( 1000 FEET IF URBAN )

**DETAIL B  
MAINLINE CLOSURE WITH POSTED DETOUR**

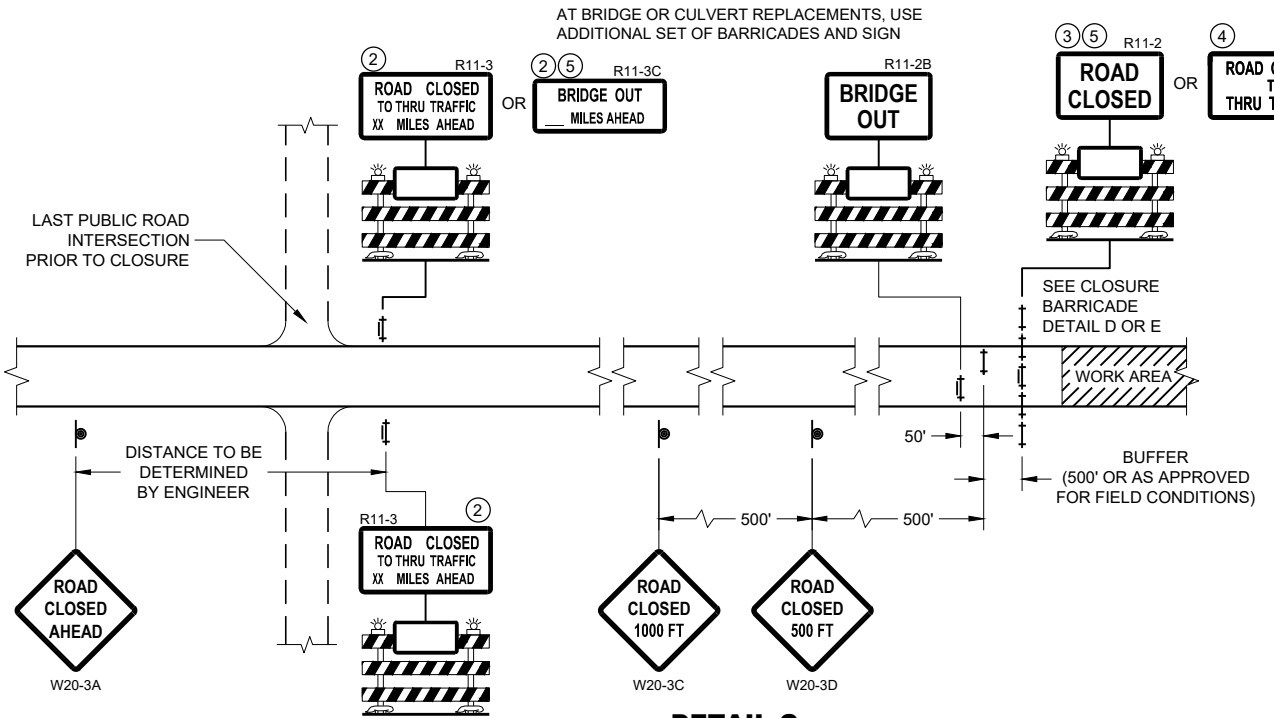
WORK ZONE LESS THAN 1/2 MILE FROM  
DETOUR ROUTE ( 1000 FEET IF URBAN )

**LEGEND**

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
- FLAGS, 16" X 16" MIN. (ORANGE)

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

- M4 - 8
- M3 - X
- M1 - 4
- M1 - 6
- M1 - 5A
- M05 - 1
- M06 - 1



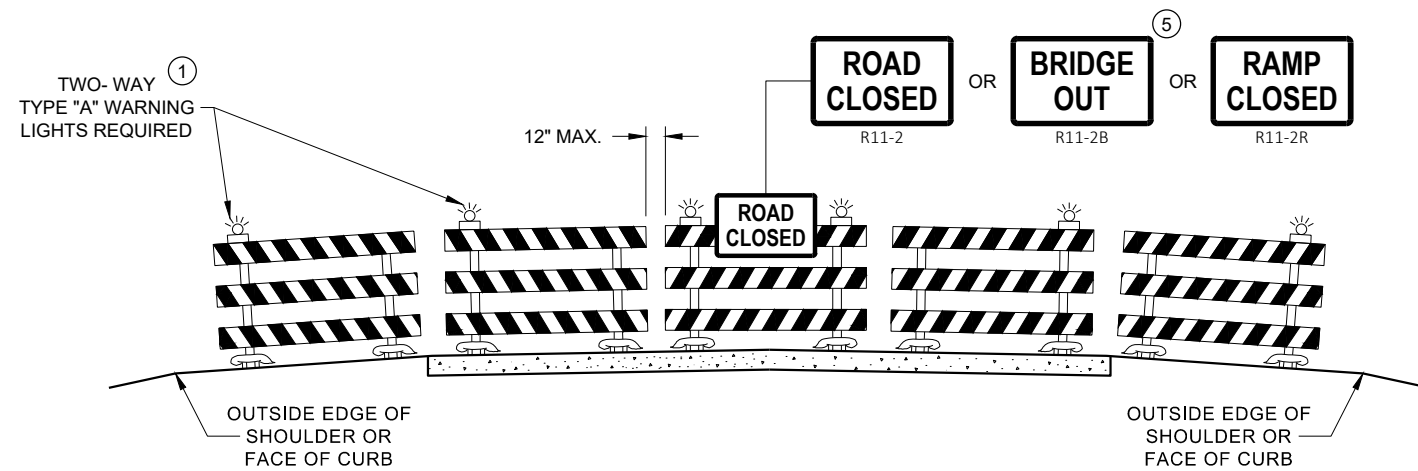
**DETAIL C  
MAINLINE CLOSURE, NO POSTED DETOUR**

SEE SDD 15C2-SHEET "b"  
FOR GENERAL NOTES  
AND FOOTNOTES ① THROUGH ⑦

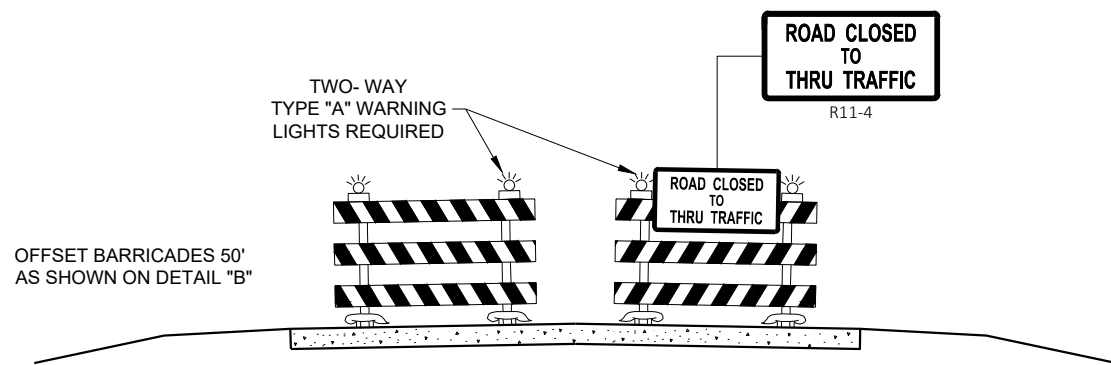
**BARRICADES AND SIGNS  
FOR MAINLINE CLOSURES**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
February 2020 /S/ Andrew Heidtke  
DATE DATE WORK ZONE ENGINEER  
FHWA



**DETAIL D  
ROAD CLOSURE BARRICADE DETAIL  
APPROACH VIEW**



**DETAIL E  
LANE CLOSURE BARRICADE DETAIL  
APPROACH VIEW**

SEE SDD 15C2 - SHEET "a" FOR LEGEND

**GENERAL NOTES**

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW - INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11 - 2, R11 - 3, M4 - 9, R11 - 4, AND R10 - 61 SIGNS PLACED ON THE BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE RAIL OR BOTTOM RAILS.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

- R11 - 2 SHALL BE 48" X 30"
- R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60" X 30"
- M4 - 9 SHALL BE 30" X 24"
- M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1 - 1 SHALL BE 36" X 36"

- ① TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT SPACING).
- ② THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.
- ③ FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".
- ④ FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".
- ⑤ FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.
- ⑥ INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.
- ⑦ "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

**BARRICADES AND SIGNS  
FOR  
VARIOUS CLOSURES**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION




APPROVED  
February 2020 /S/ Andrew Heidtke  
DATE WORK ZONE ENGINEER

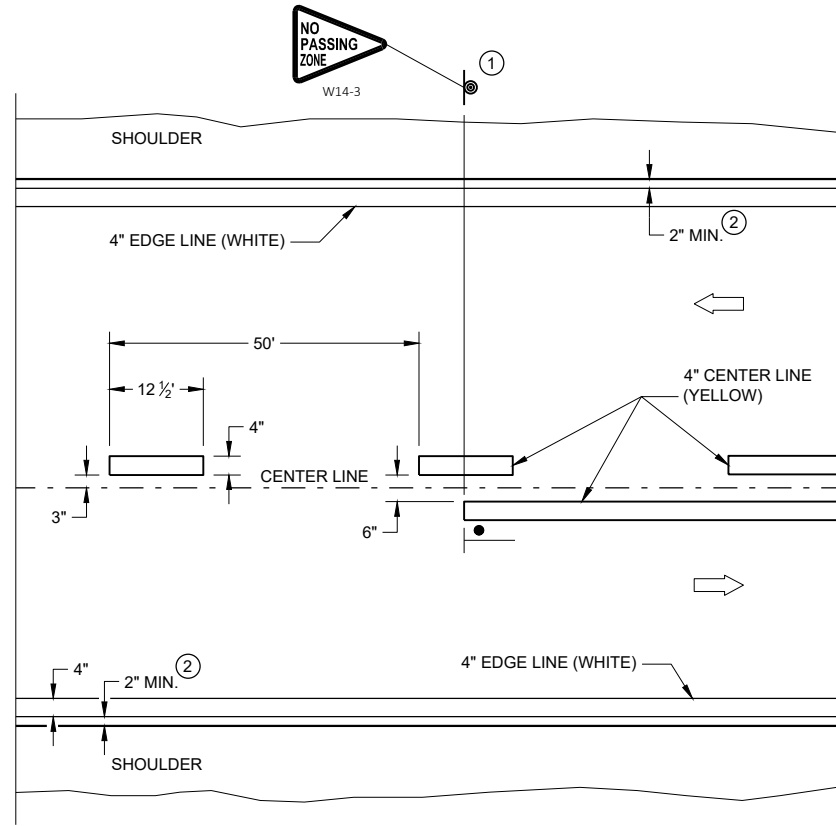
**GENERAL NOTES**

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

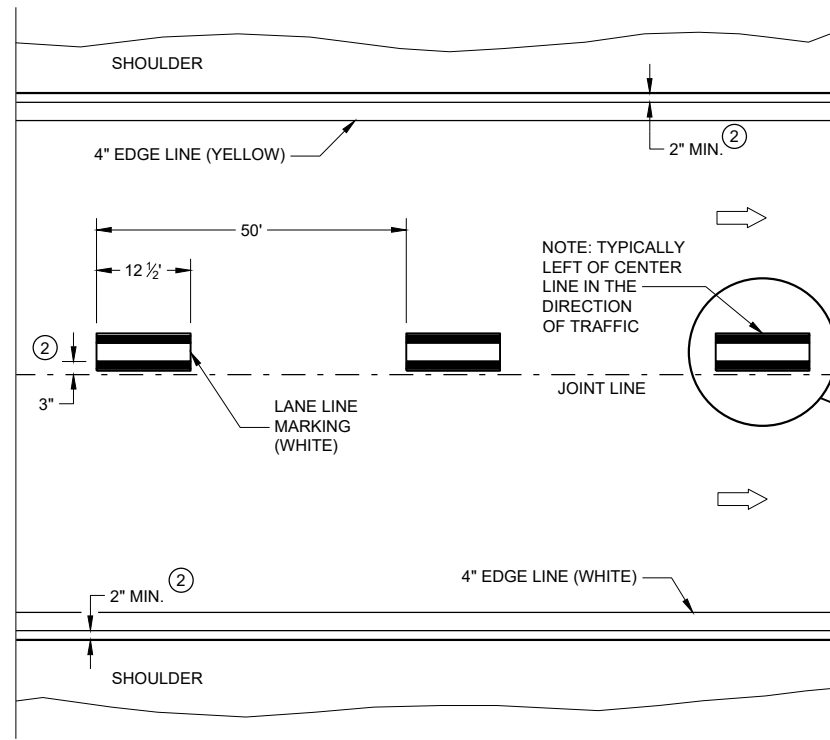
- ① LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING
- ② MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

**LEGEND**

-  "T" MARKING
-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC

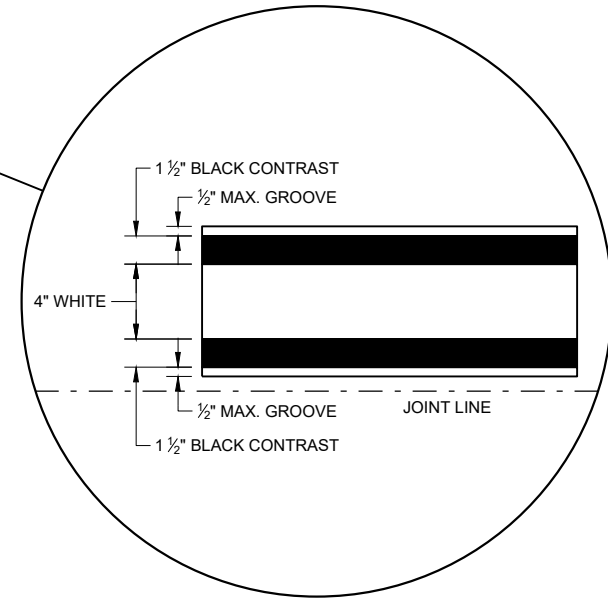


**TWO WAY TRAFFIC**



**ONE WAY TRAFFIC**

**PERMANENT PAVEMENT MARKING**



**PERMANENT LONGITUDINAL PAVEMENT MARKINGS**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
DATE: May 2022 /S/ Jeannie Silver  
STATEWIDE SIGNING AND MARKING ENGINEER



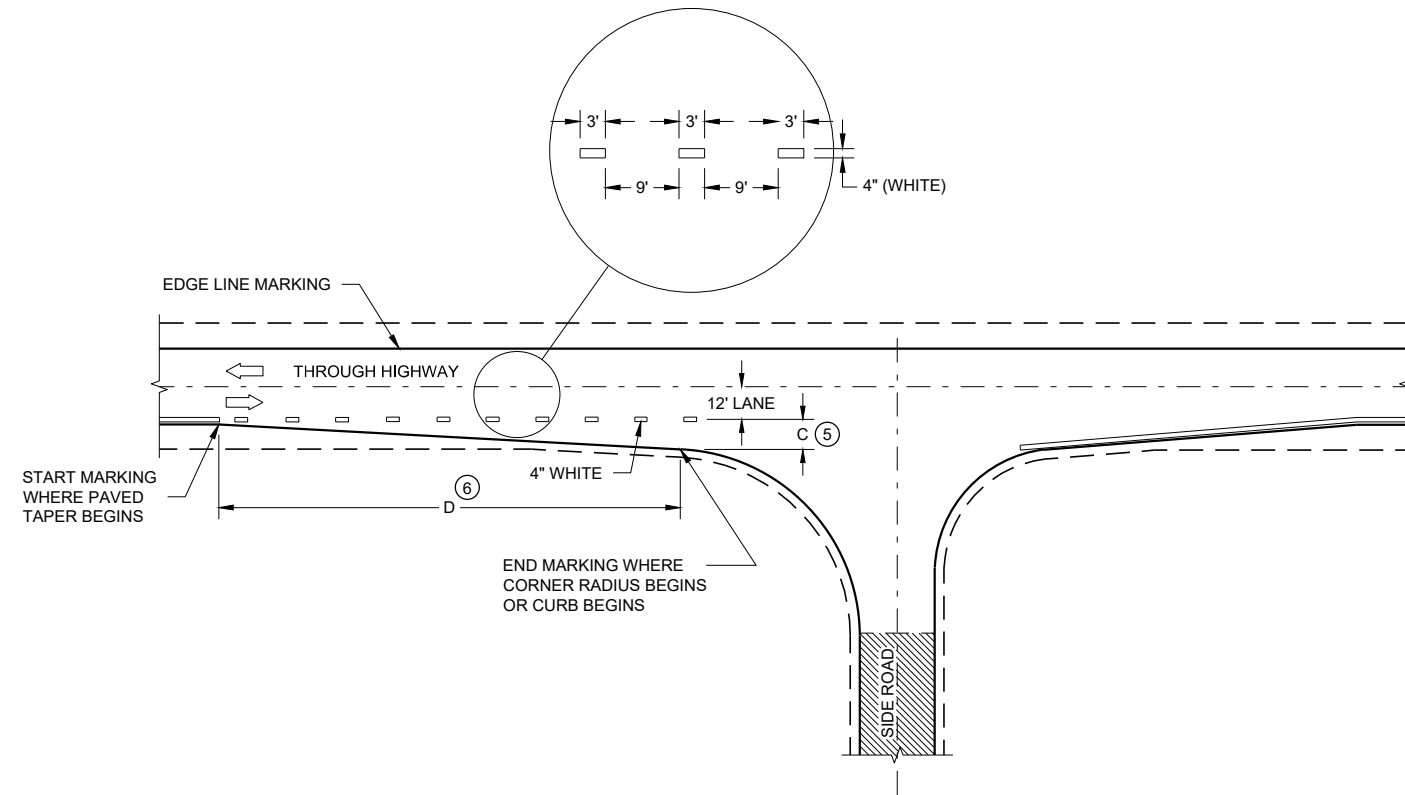
**GENERAL NOTES**

OMIT EDGE LINES THROUGH INTERSECTIONS. CONTINUE EDGE LINES THROUGH DRIVEWAYS.

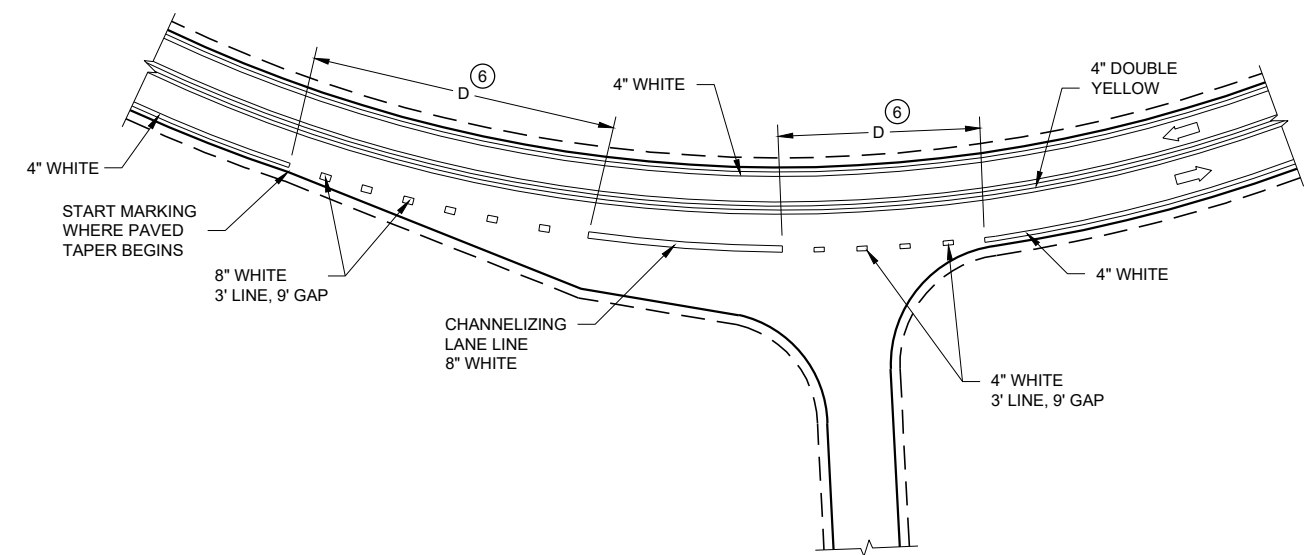
- ① WHEN DISTANCE "A" IS LESS THAN 250 FEET, OMIT LANE LINE.
- ② WHEN DISTANCE "B" IS LESS THAN 100 FEET, OMIT CHANNELIZING LANE LINE.
- ③ BARRIER LINE ENDS AT SIDE ROAD PAVEMENT / SURFACE EDGE EXTENSION.
- ④ BARRIER LINE STARTS 500 FEET PRIOR TO THE BYPASS TAPER.
- ⑤ WHEN DISTANCE "C" IS LESS THAN 4 FEET, OMIT DOTTED EXTENSION.
- ⑥ WHEN DISTANCE "D" IS LESS THAN 50 FEET, OMIT DOTTED EXTENSION.

**LEGEND**

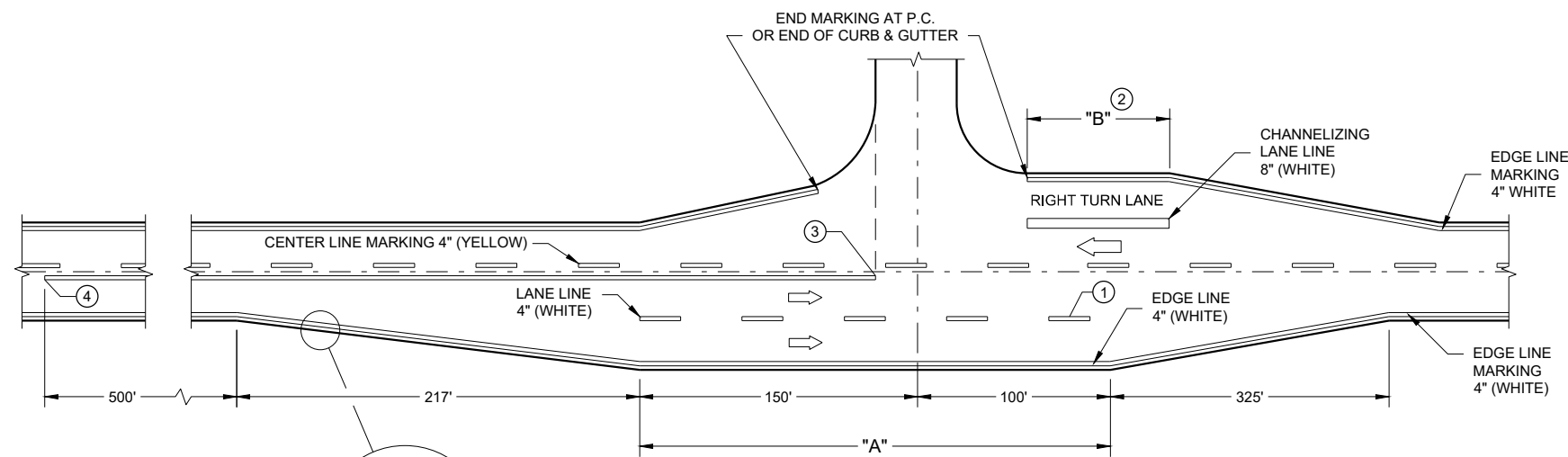
➡ DIRECTION OF TRAVEL



**MINOR INTERSECTION**



**INTERSECTION ON OUTSIDE OF CURVE**



**MAJOR INTERSECTIONS  
(INTERSECTION WITH FULL RIGHT TURN LANE OR BYPASS LANE)**

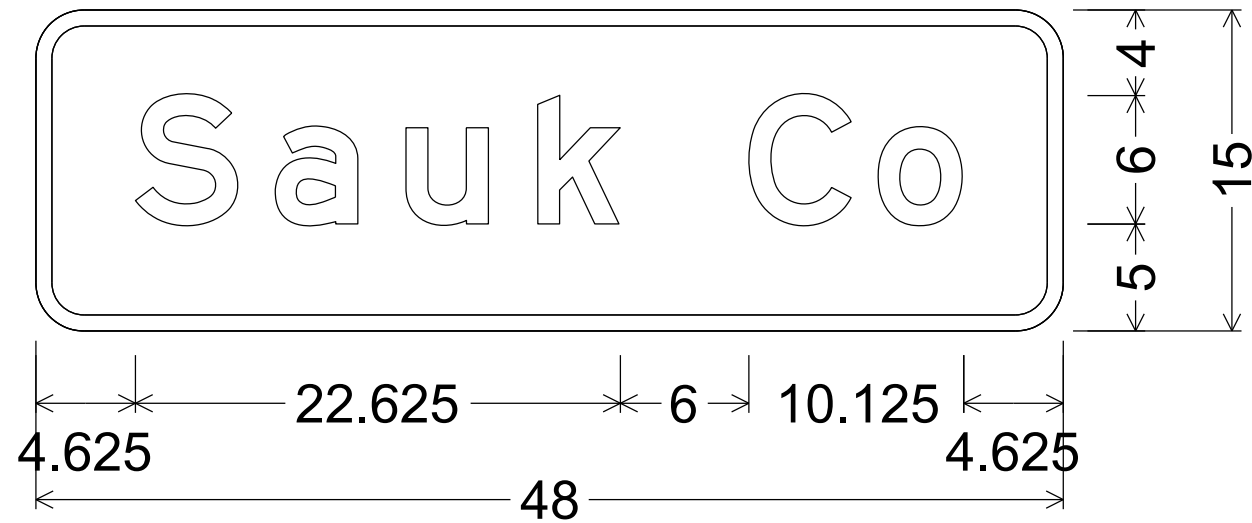
BYPASS LANE PAVED SHOULDER WIDTH (AS SHOWN ELSEWHERE IN PLANS) - PLUS 2 INCHES

**PAVEMENT MARKING  
(INTERSECTIONS)**

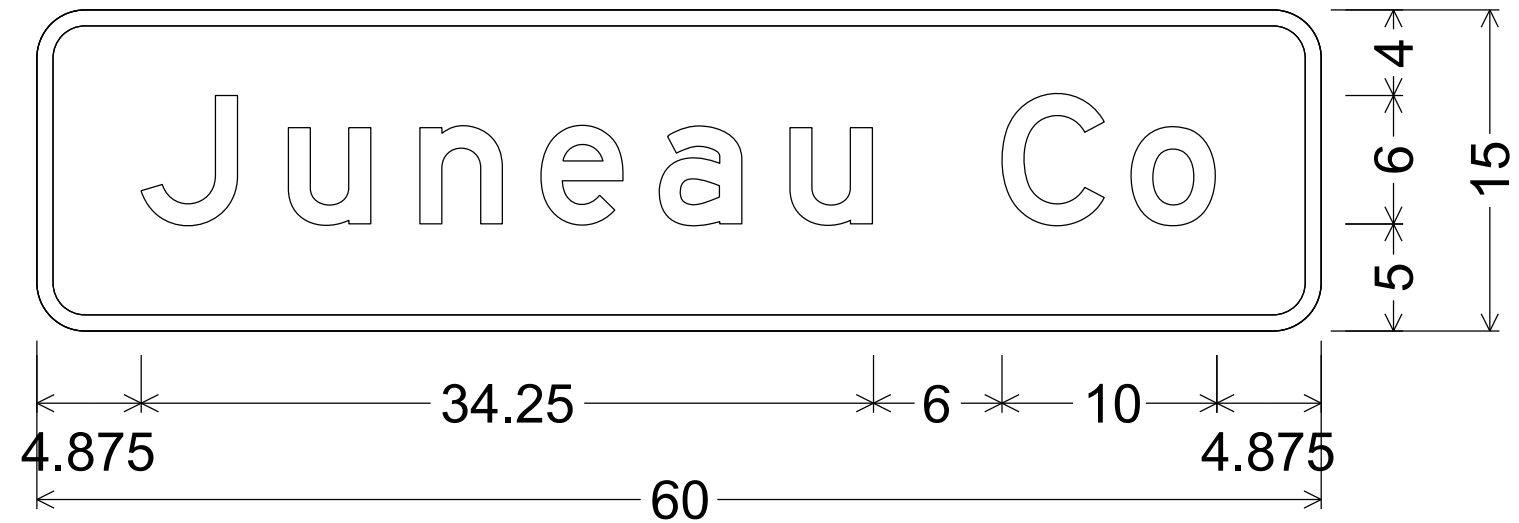
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

NOTES

1. Signs are Type II - Type H Reflective
2. Color:  
Background - Green  
Message - White
3. Message Series - E



I2-2; 2.250" Radius, 0.750" Border

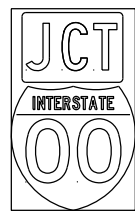


I2-2; 2.250" Radius, 0.750" Border

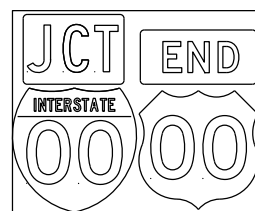
7

7

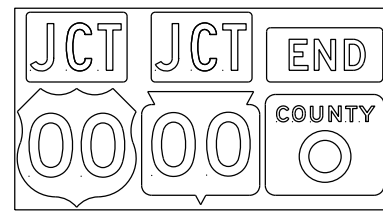
# TYPICAL ASSEMBLIES



J1-1



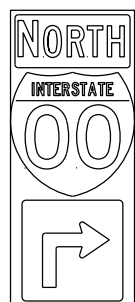
J1-2



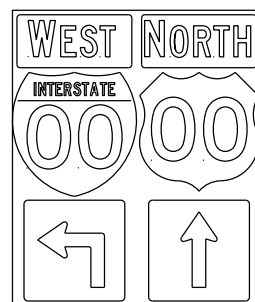
J1-3



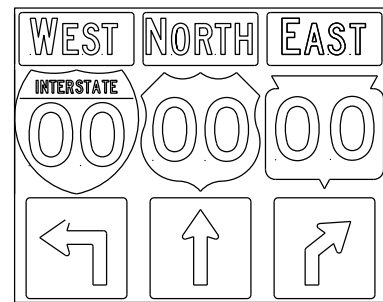
JR1-1



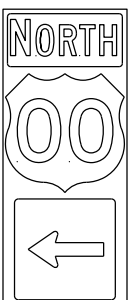
J2-1



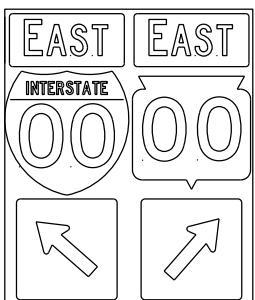
J2-2



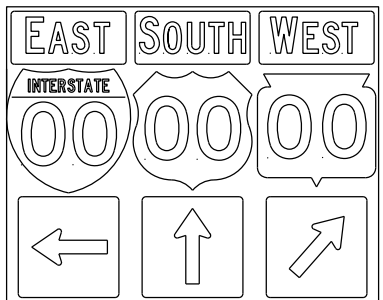
J2-3



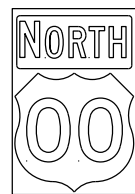
J3-1



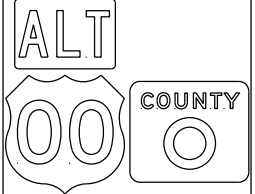
J3-2



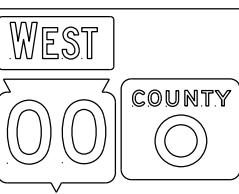
J3-3



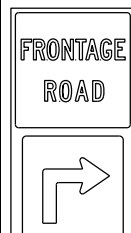
J4-1



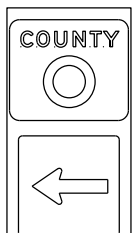
J4-2



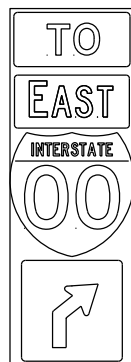
J4-2



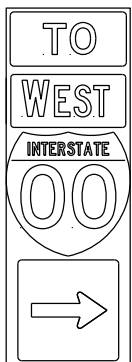
J12-1



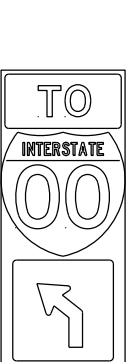
J13-1



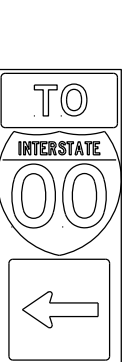
J32-1



J33-1



J22-1



J23-1



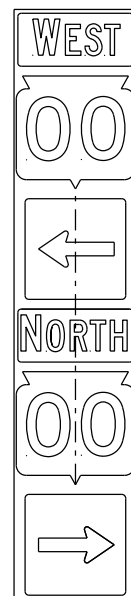
JR13-1



JR23-1

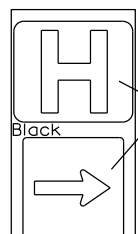


JR99-1



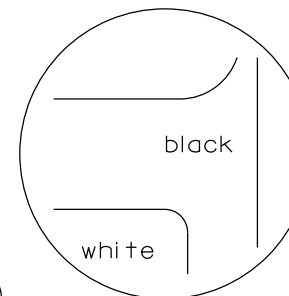
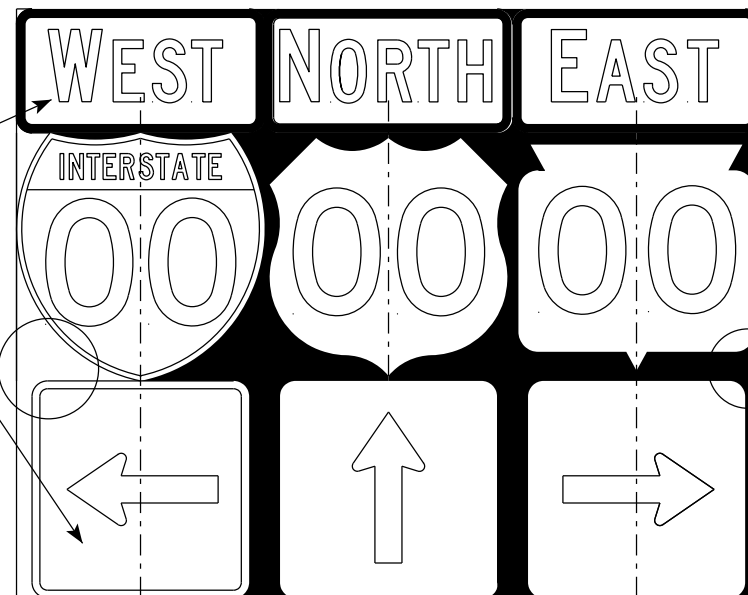
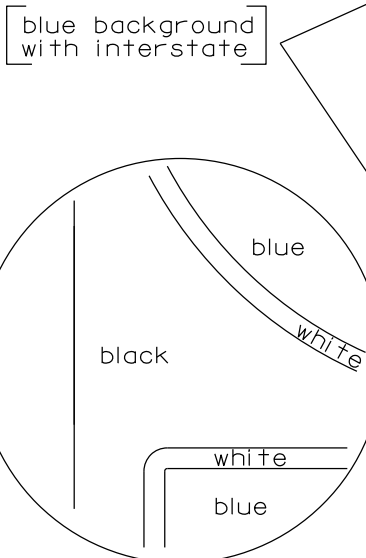
JV

(Typical Vertical J-Assembly See Note 10 and 11)



JH-1

Blue Background



black background

## NOTES

- Signs are Type II - Type H Reflective
- Color:
  - Background - Black Non-reflective
  - Message - see Note 5
- Message Series - See Note 5
- Corners shall be square or rounded if base material is plywood. If base material is metal the corners shall be rounded.
- The colors and message spacing on each marker shall be according to the applicable route marker panel specifications.
- Certain marker heads require the component pieces to be the same color. As an example, all the components used with an MI-1 Interstate marker shall be blue.
- Single panel j-assemblies shall only be used with route marker shields that are same size. If the route marker shields are different size use multiple piece component.
- Route assemblies that have 24 inch route shields and have dimensions greater than 48 inches (both vertical and horizontal) shall have one horizontal splice between the arrows and route shields. Vertical splices shall not be used on route assemblies with a horizontal dimension of 144 inches or less. The contractor shall not use more than one vertical joint per sign and the joint shall be between route shields.
- Route assemblies that have 36 inch shields and have dimensions greater than 48 inches (both vertical and horizontal) shall have two horizontal splices. One horizontal splice shall be between the cardinal direction and route shields and the other horizontal splice shall be between the arrows and route shields. Vertical splices shall not be used on route assemblies with a horizontal dimension of 144 or less. The contractor shall not use more than one vertical joint per sign and the joint shall be between route shields.
- All Vertical J Assemblies are given a Sign Code of JV
- For JV Assemblies that have a mixture of Interstate and non Interstate shields, arrows and cardinals shall be white on blue.

### ROUTE MARKERS & COMPONENTS IN TYPICAL ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

*Matthew R. Rauch*  
for State Traffic Engineer

DATE 3/18/21

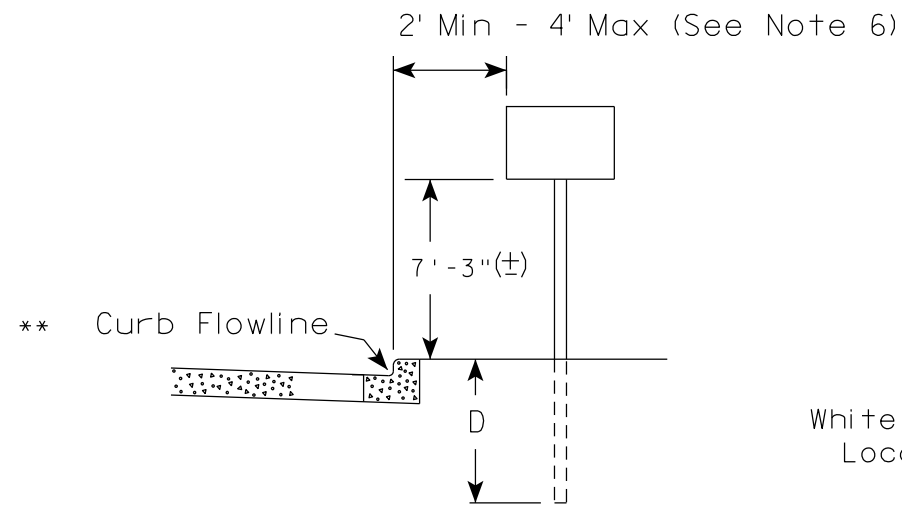
PLATE NO. A2-1S.9

PROJECT NO:

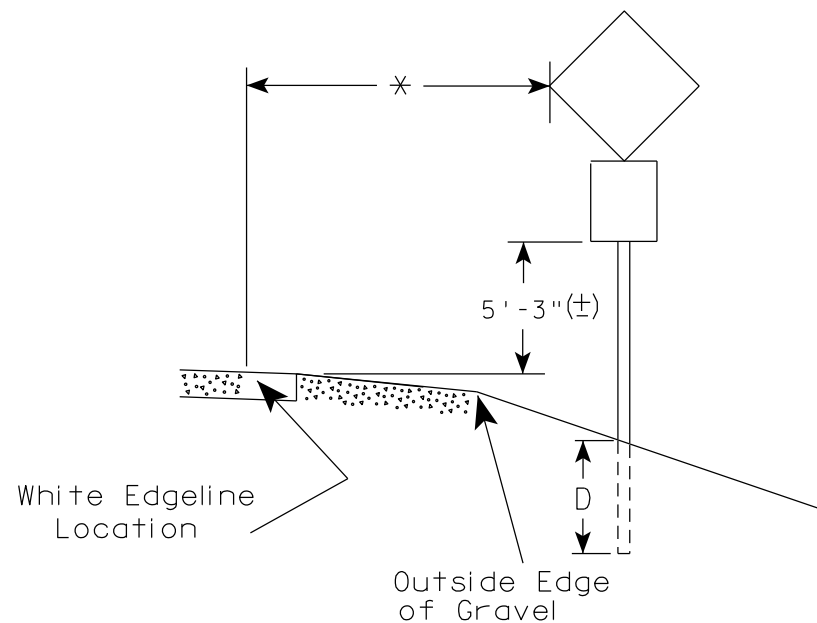
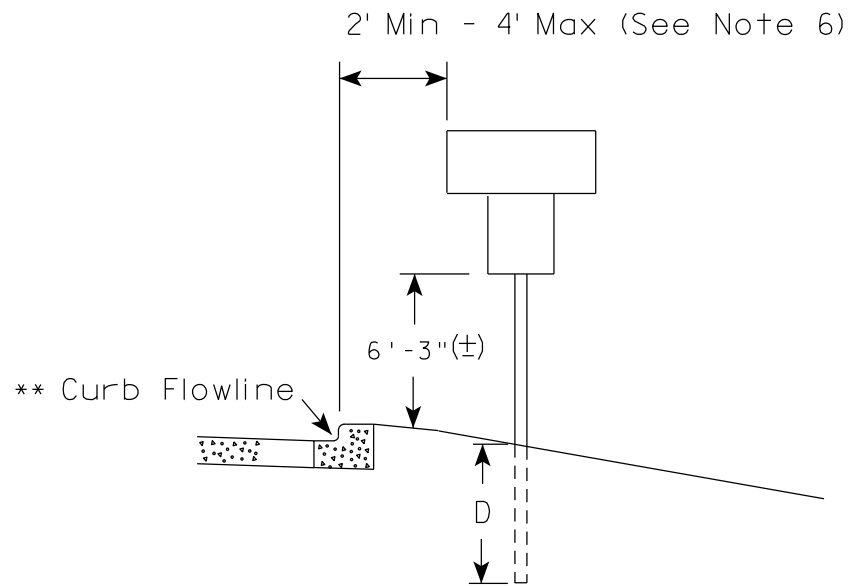
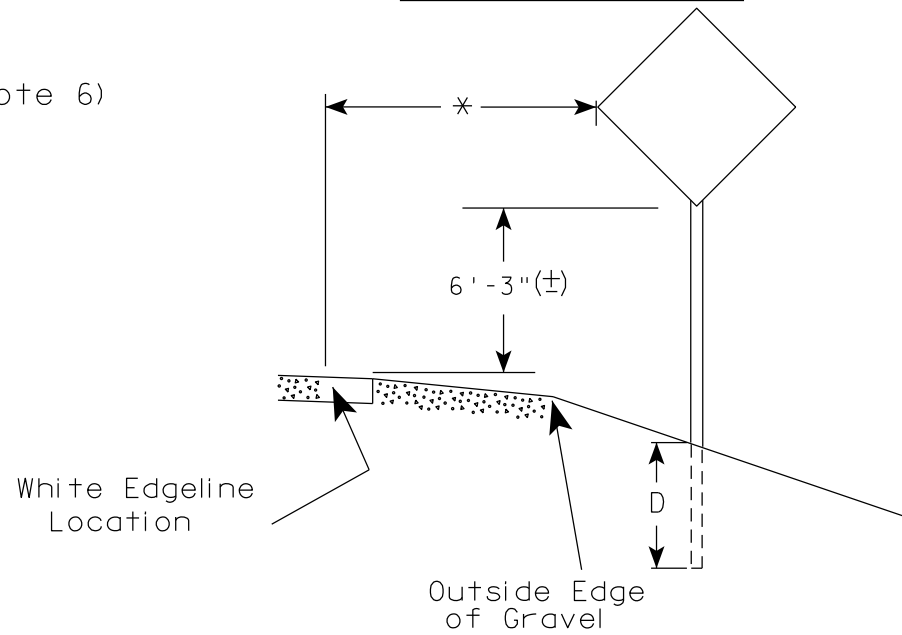
SHEET NO:

E

URBAN AREA



RURAL AREA (See Note 2)



POST EMBEDMENT DEPTH

Area of Sign Installation ( Sq. Ft. )	D ( Min )
20 or Less	4'
Greater than 20	5'

GENERAL NOTES

1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on or behind barrier wall, see A4-10 sign plate.  
The Double Arrow sign (W12-1D) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).
3. For expressways and freeways, mounting height is 7'- 3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
4. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±).
5. Offset distance shall be consistent with existing signs or consistent throughout length of project.
6. The (±) tolerance for mounting height is 3 inches.
7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the Engineer.

\* \* The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

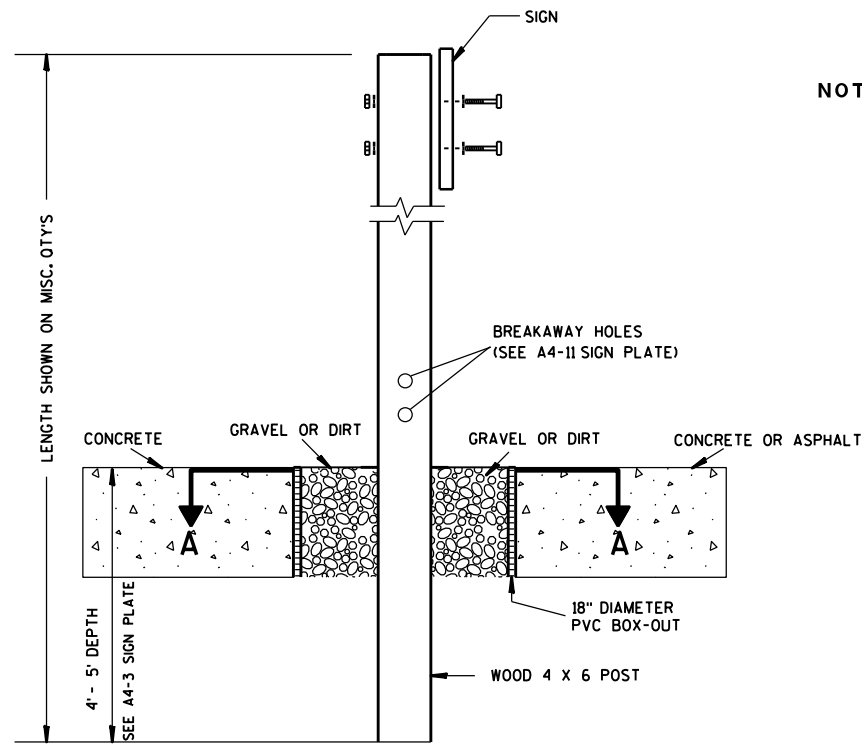
\* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

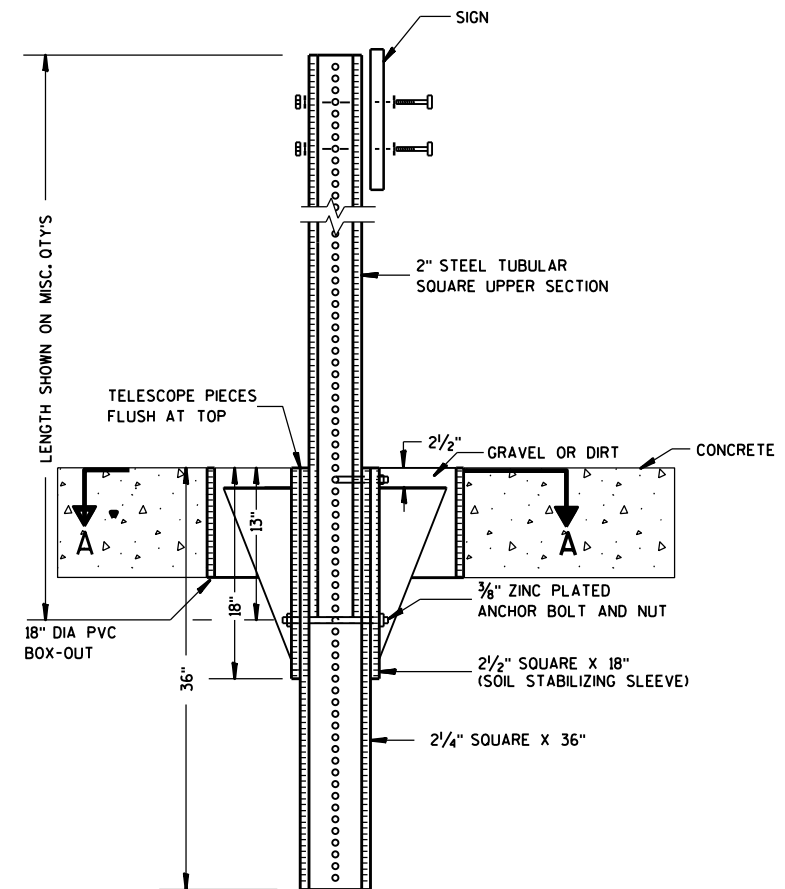
DATE 5/13/2020 PLATE NO. A4-3.22



**ELEVATION VIEW**

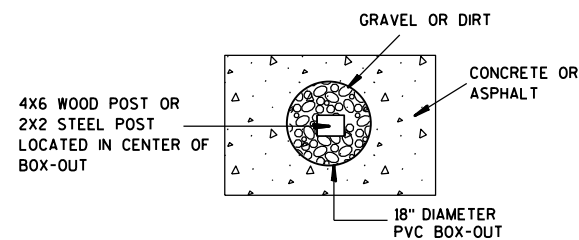
**DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT**

- NOTES:**
1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
  2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
  3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



**ELEVATION VIEW**

**DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT**



**PLAN VIEW**

**FOR NEW CONCRETE/ ASPHALT INSTALLATIONS**

**SIGN POST  
BOX-OUTS  
A4-3B**

WISCONSIN DEPT OF TRANSPORTATION

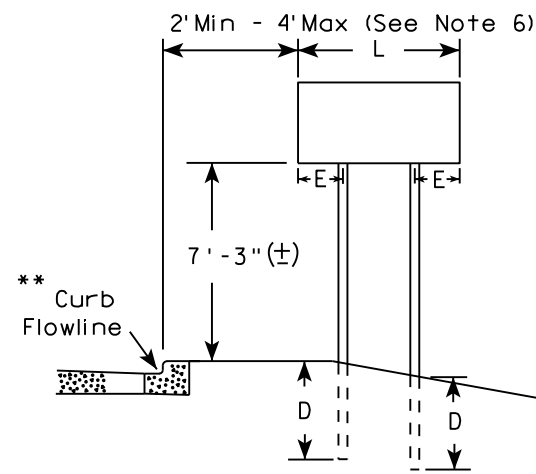
APPROVED *Matthew R Rauch*  
for State Traffic Engineer

DATE 1/27/14 PLATE NO. A4-3B.1

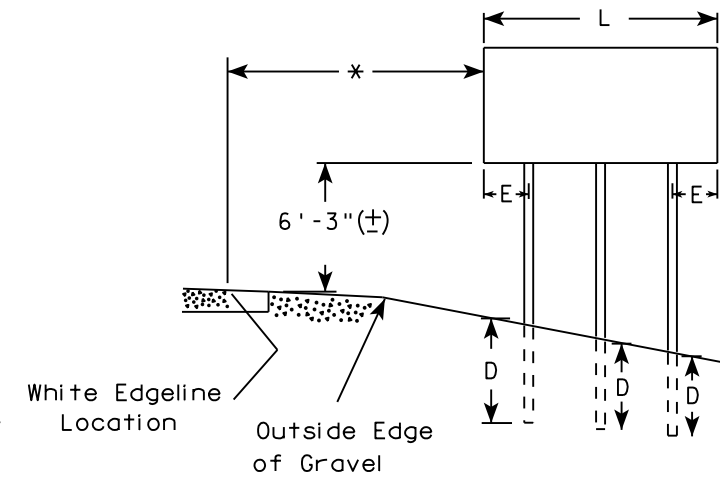
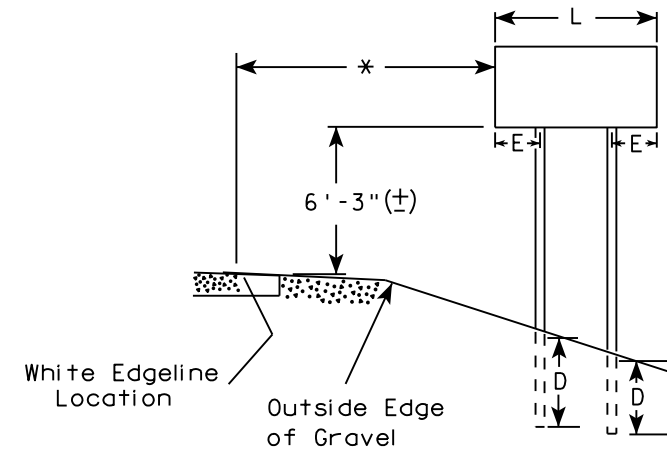
GENERAL NOTES

1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
2. See tables below for required number of posts.
3. For expressways and freeways, mounting height is 7'-3" (±) or 6'-3" (±) depending upon existence of sub-sign.
4. The (±) tolerance for mounting height is 3 inches.
5. J-Assemblies are considered to be one sign for mounting height.
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the engineer.
8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).

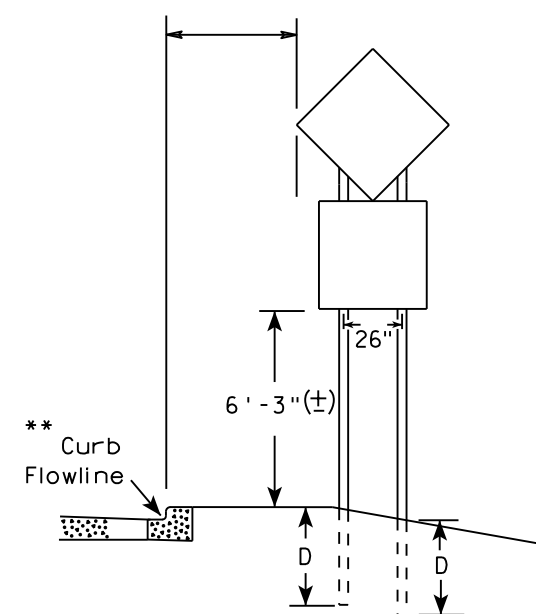
URBAN AREA



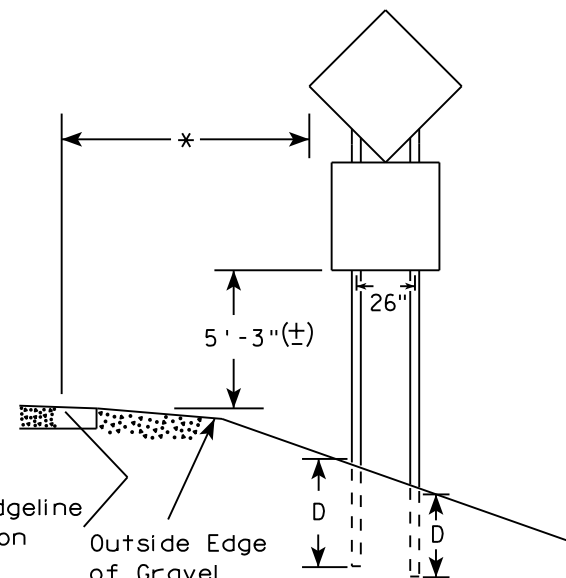
RURAL AREA (See Note 3)



2' Min - 4' Max (See Note 6)



48" DIAMOND WARNING SIGN



48" DIAMOND WARNING SIGN

\* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

\*\* The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

\*\*\* See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)	
L	E
Greater than 48" Less than 60"	12"
60" to 108"	L/5

\*\*\*

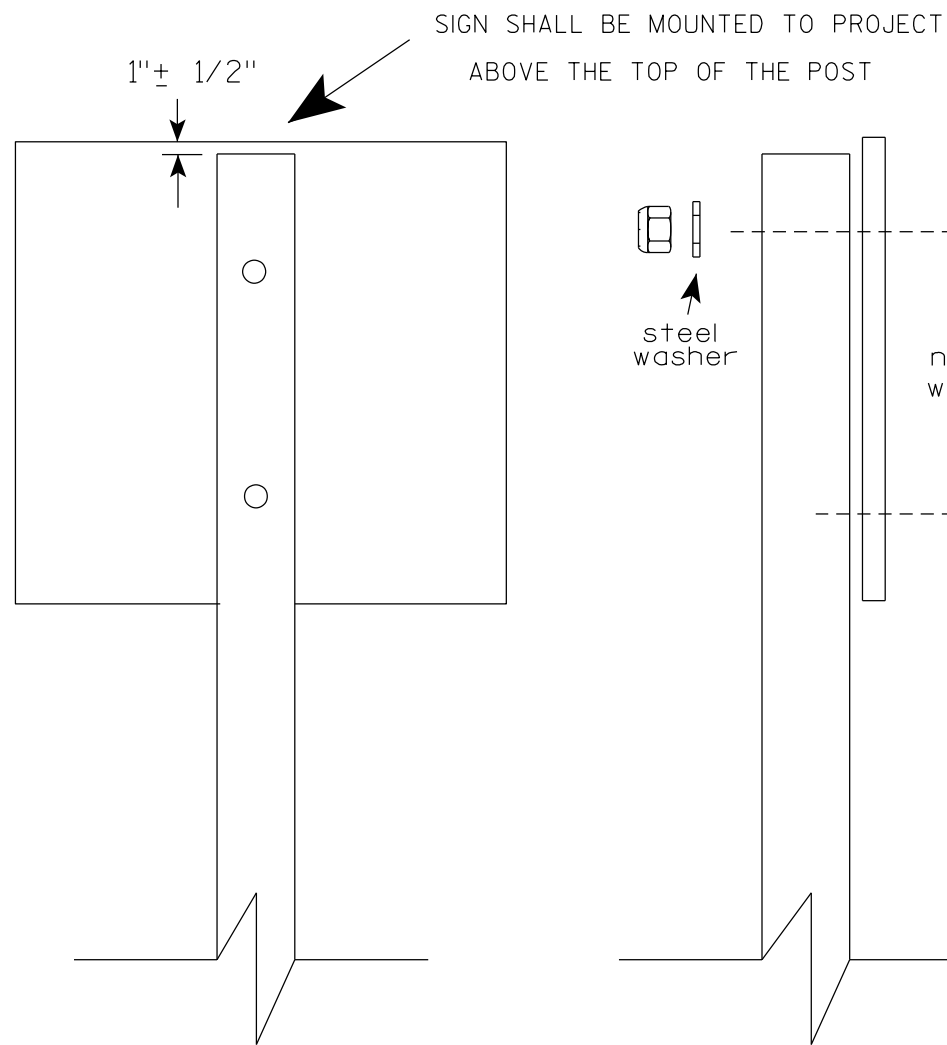
SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)	
L	E
Greater than 108" to 144"	12"

POST EMBEDMENT DEPTH

Area of Sign Installation ( Sq. Ft. )	D ( Min )
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS

WISCONSIN DEPT OF TRANSPORTATION  
 APPROVED *Matthew R. Rauch*  
 For State Traffic Engineer  
 DATE 8/21/17 PLATE NO. A4-4.15



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

- Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
- Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

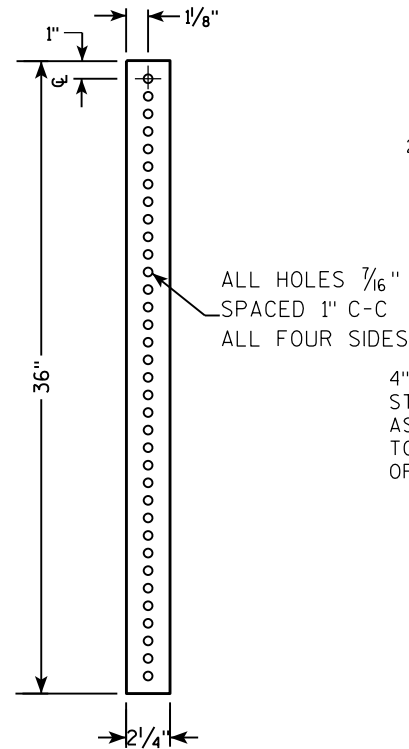
- STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)
- MACHINE BOLTS -  $\frac{5}{16}$ " X 1-3/4" Length w/ lock nuts
- WOOD POSTS (4" x 6")
- LAG SCREWS -  $\frac{3}{8}$ " X 3" (NO STRINGERS ON BACK OF SIGN)
  - $\frac{3}{8}$ " X 4" (STRINGERS ON BACK OF SIGN)
- SQUARE STEEL POSTS (2" x 2")
- MACHINE BOLTS -  $\frac{3}{8}$ " X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)
  - $\frac{3}{8}$ " X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)
- RIVETS -  $\frac{9}{32}$ " (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
- O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH
- WASHERS (ALL POSTS) -
- 1-1/4" O.D. X  $\frac{3}{8}$ " I.D. X  $\frac{1}{16}$ " STEEL
  - 1-1/4" O.D. X  $\frac{3}{8}$ " I.D. X .080 NYLON

\* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

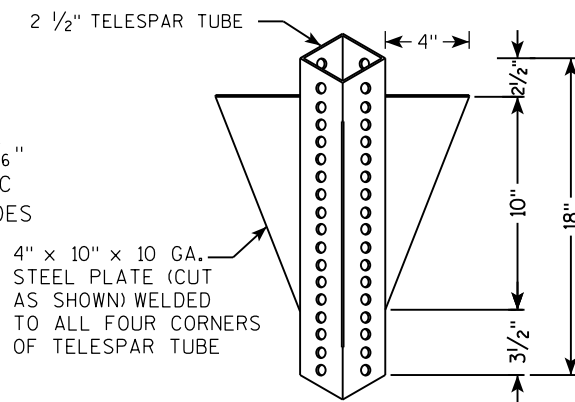
ATTACHMENT OF SIGNS TO POSTS	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R Rauch</i> For State Traffic Engineer
DATE 4/1/2020	PLATE NO. A4-8.9

**TELESCOPIC TUBING ANCHORS  
TWO PIECE SYSTEM**

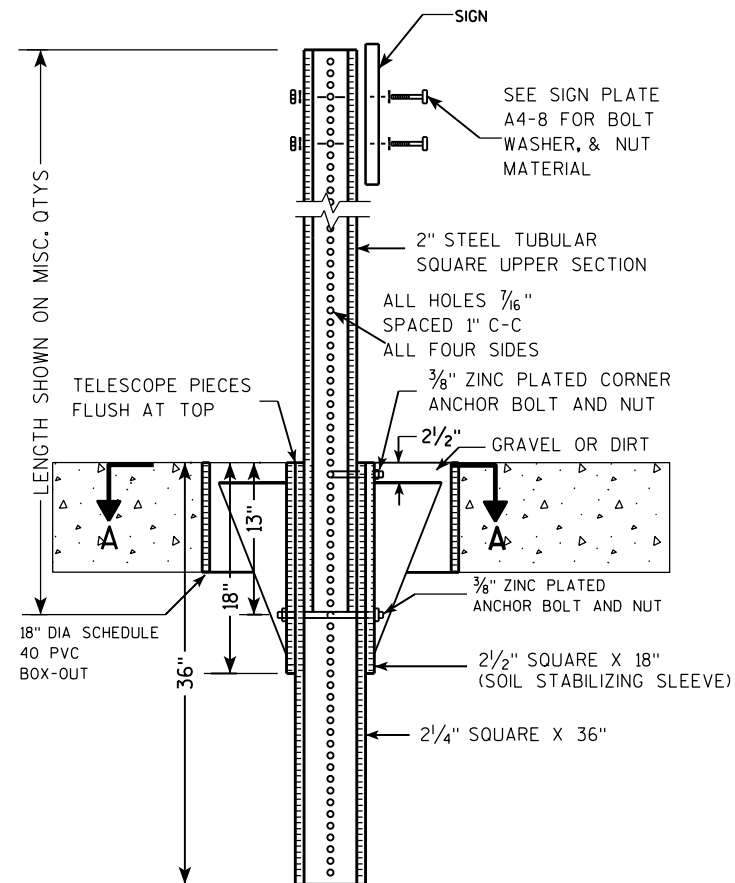
**2 1/4" SQUARE  
12 GAUGE  
PERFORATED  
GALVANIZED FINISH**



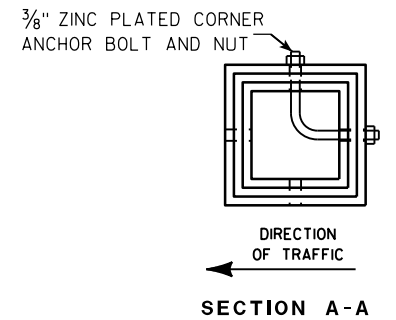
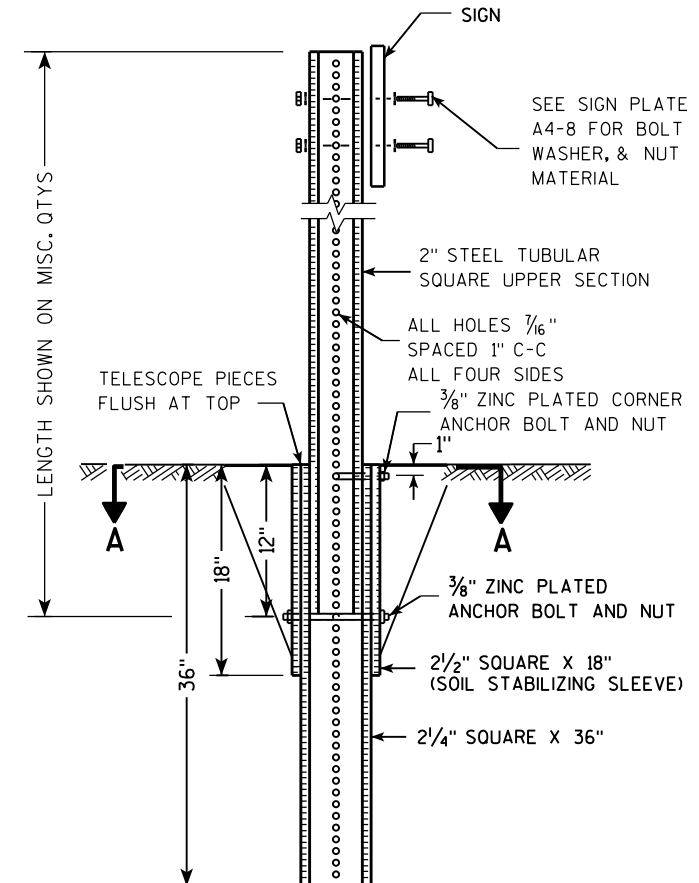
**2 1/2" SQUARE  
12 GAUGE  
OMNI-DIRECTIONAL  
PERFORATED  
SOIL STABILIZING SLEEVE  
GALVANIZED FINISH**



**DETAIL OF TUBULAR STEEL SIGN POST  
(IN POURED CONCRETE OR ASPHALT)**



**DETAIL OF TUBULAR STEEL SIGN POST  
(IN LOCATIONS OTHER THAN POURED CONCRETE OR ASPHALT)**



Area of Sign Installation (Sq. Ft.)	Number of Required Posts
9 or less	1
Greater than 9 less than or equal to 18	2
Greater than 18 less than or equal to 27	3

Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

**TUBULAR STEEL  
SIGN POST  
A4-9**

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 2/05/15 PLATE NO. A4-9.9

PROJECT NO:

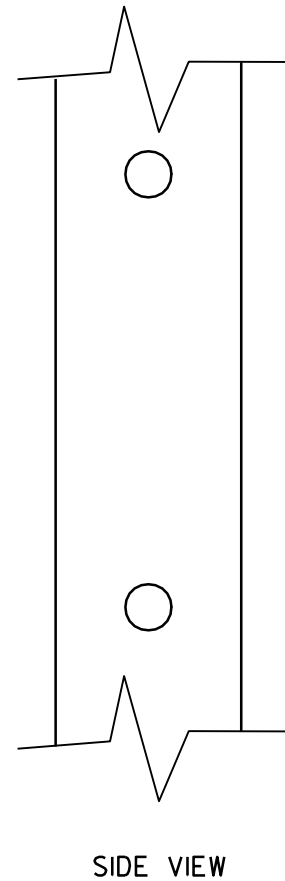
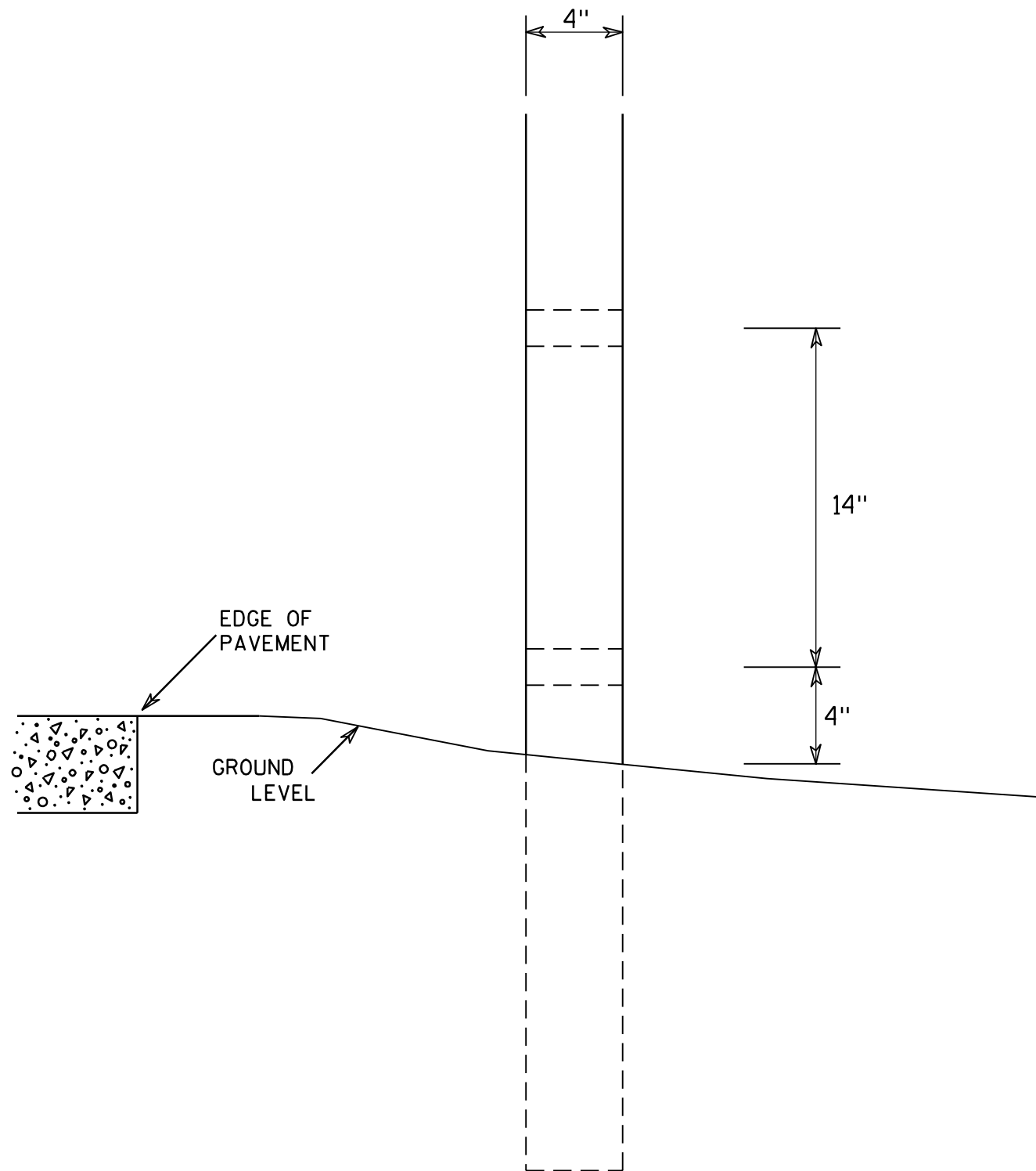
HWY:

COUNTY:

SHEET NO:

E






GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

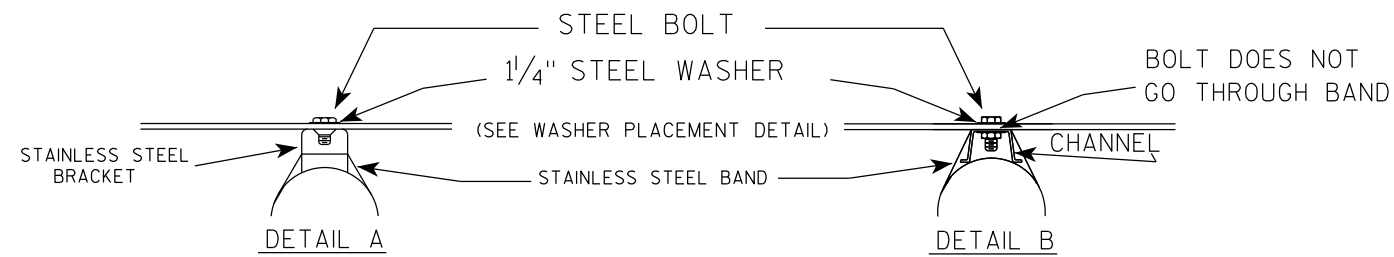
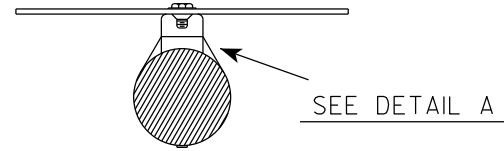
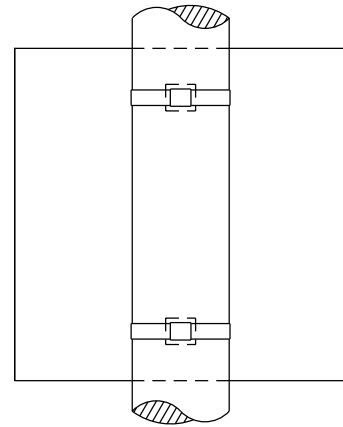
7

7

<b>4 X 6 WOOD POST MODIFICATIONS</b>	
<i>WISCONSIN DEPT OF TRANSPORTATION</i>	
APPROVED	 <small>for State Traffic Engineer</small>
DATE <u>3/27/97</u>	PLATE NO. <u>A4-11.2</u>

# BANDING

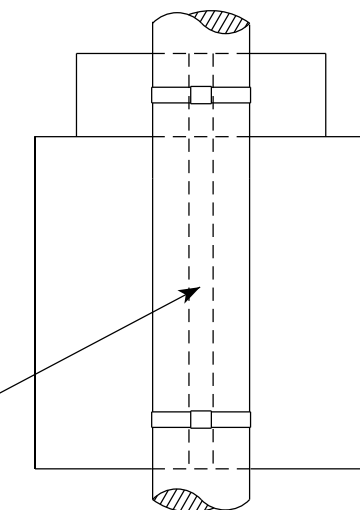
SINGLE SIGN



## GENERAL NOTES

1. Any sign over 3 feet in width shall use the V-Block banding method. See A5-10 standard plate.
2. Signs 3 feet or greater in height shall have three bracket bands installed. Signs less than 3 feet in height shall have two bracket bands installed.
3. Banding and assembly bracket shall be stainless steel. All bands shall be 3/4" in width and 0.025" thickness.
4. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
  - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
  - b. Electro-galvanized in accordance with ASTM designation: B 633, Type III, SC 3

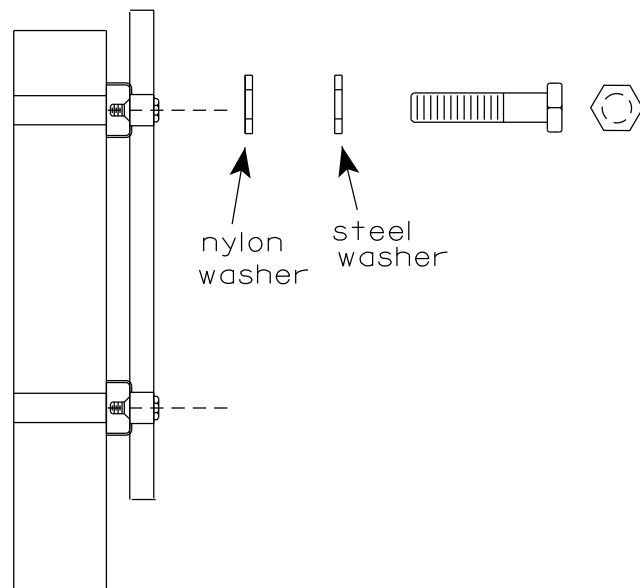
"J" ASSEMBLY



CHANNEL  
SEE TYPICAL PANEL  
INSTALLATION SHEET



WASHER PLACEMENT



WASHERS (ALL POSTS) -  
1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL  
1-1/4" O.D. X 3/8" I.D. X .080 NYLON  
FOR ALL TYPE H SIGNS

STANDARD SIGN  
SIGN BANDING DETAILS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 6/10/19 PLATE NO. A5-9.4

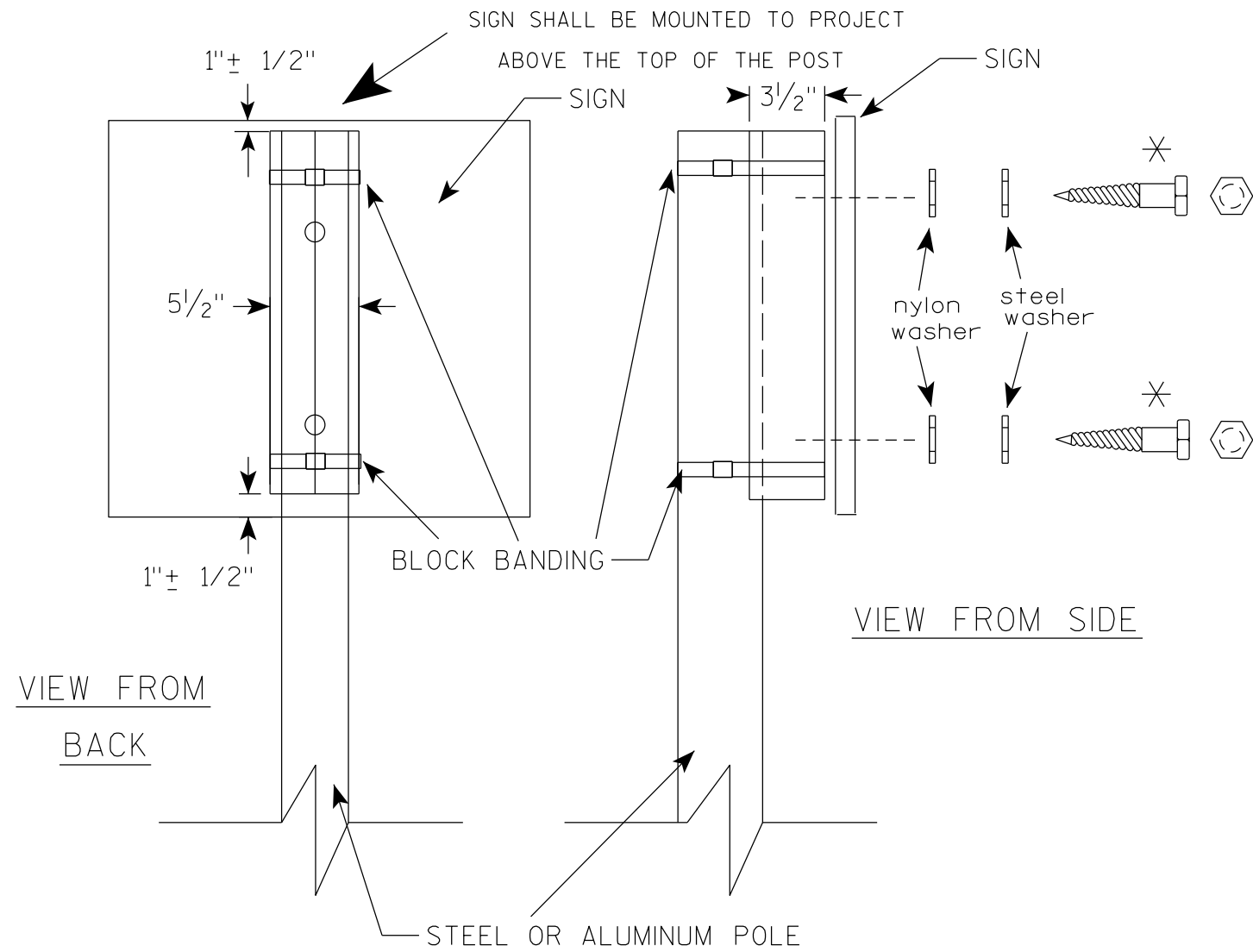
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

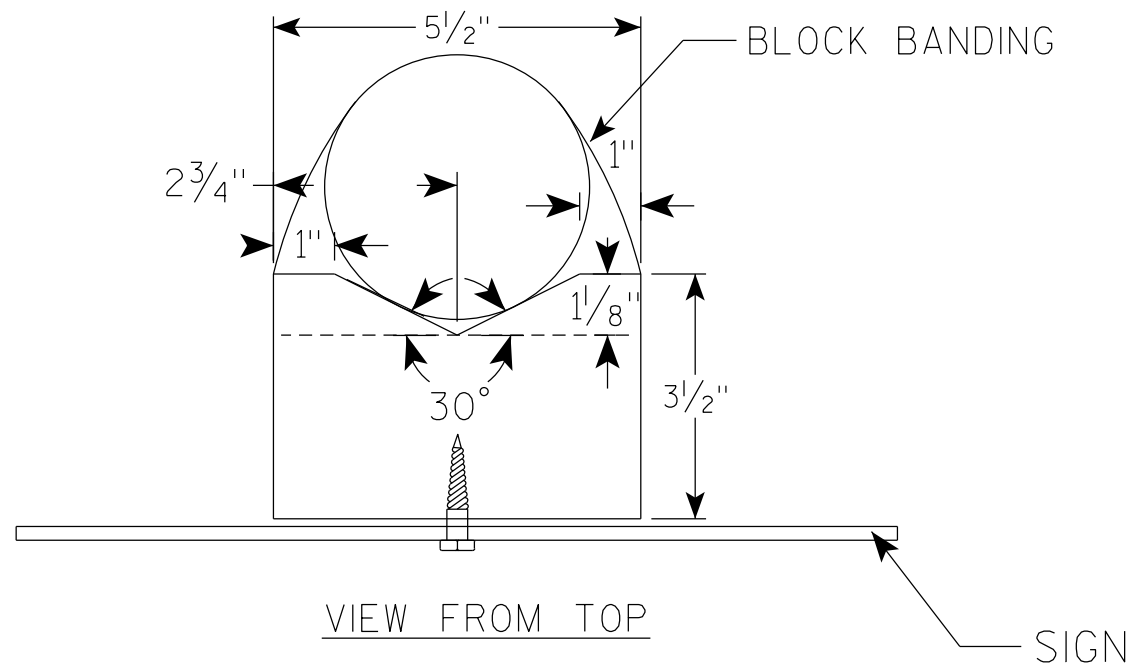
E



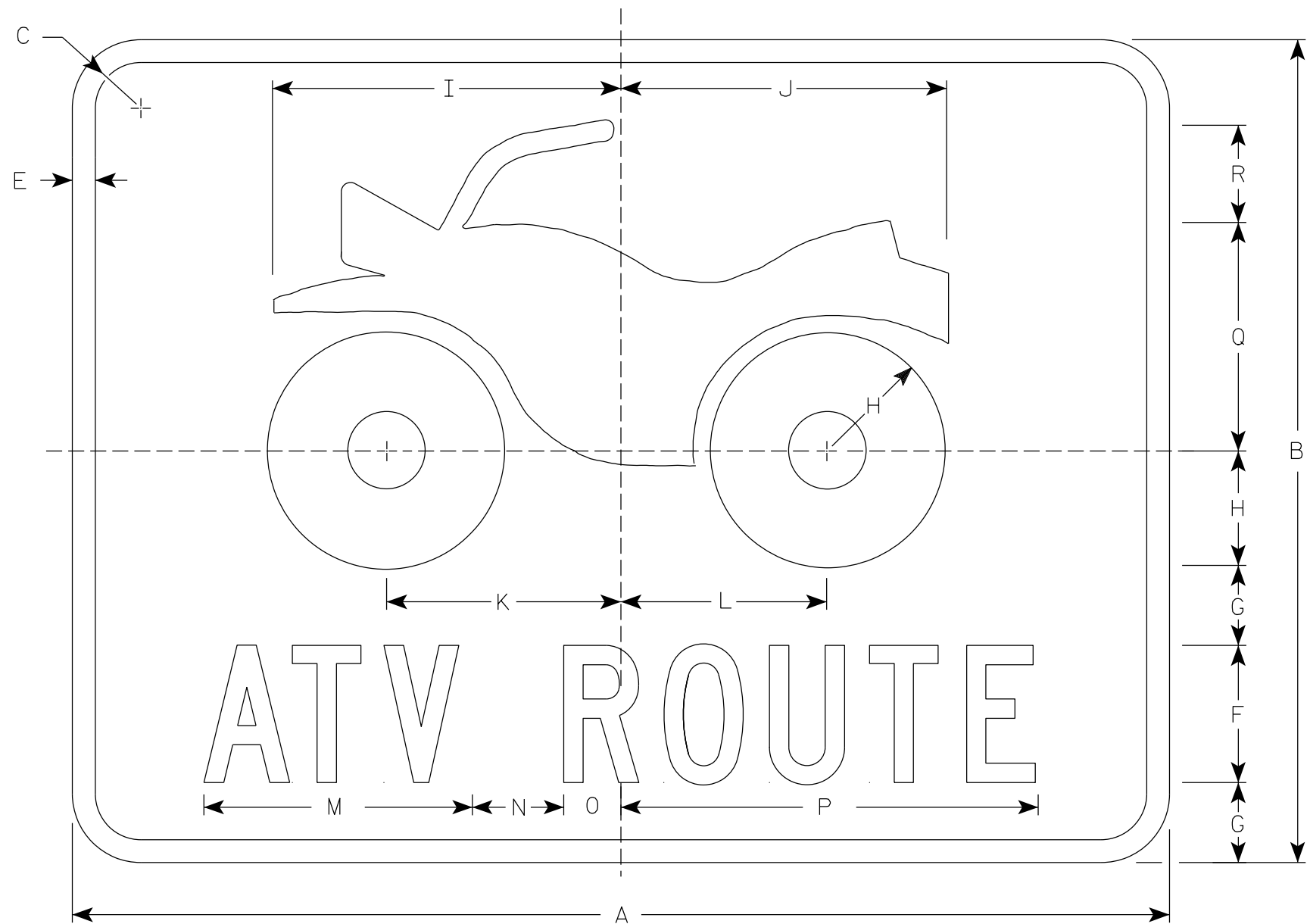
GENERAL NOTES

1. WOOD 4"X6" POST MATERIAL SHALL CONFORM TO 507.2.2 OF THE WISDOT STANDARD SPECIFICATIONS
2. BLOCK BANDING AND CLIPS SHALL BE STAINLESS STEEL, 3/4" WIDTH AND 0.025" THICKNESS
3. SIGNS 3' OR GREATER IN HEIGHT SHALL UTILIZE 3 BLOCK BANDS. SIGNS UNDER 3' IN HEIGHT SHALL UTILIZE 2 BLOCK BANDS
4. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA, BUT NORMALLY THERE ARE TWO. FOR SIGNS GREATER THAN 9 S.F. 3 FASTENERS SHALL BE USED.
5. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
  - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
  - b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3
6. ALL BOLTS SHALL HAVE HEXAGONAL HEADS.
7. STEEL WASHERS SHALL BE 1/4" O.D. X 3/8" I.D. X 1/16"
8. NYLON WASHERS SHALL BE 1/4" O.D. X 3/8" I.D. X .080 FOR TYPE H OR TYPE F FACE SIGN

✱ LAG BOLTS SHALL BE 3/8" X 2 1/2"



BLOCK BANDING DETAIL ( V-BLOCK OPTION )	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R Rauch</i> for State Traffic Engineer
DATE 4/19/2022	PLATE NO. A5-10.3



- NOTES
1. Sign is Type II - Type H Reflective
  2. Color:  
     Background - Green  
     Message - White
  3. Message Series - C

D11-10

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	18	1 1/8		1/2	3	1 3/4	2 1/2	7 5/8	7 1/8	5 1/8	5 1/2	5 7/8	2	1 1/4	9 1/8	5	2 1/8									3.0
3																											
4																											
5																											

STANDARD SIGN  
D11-10

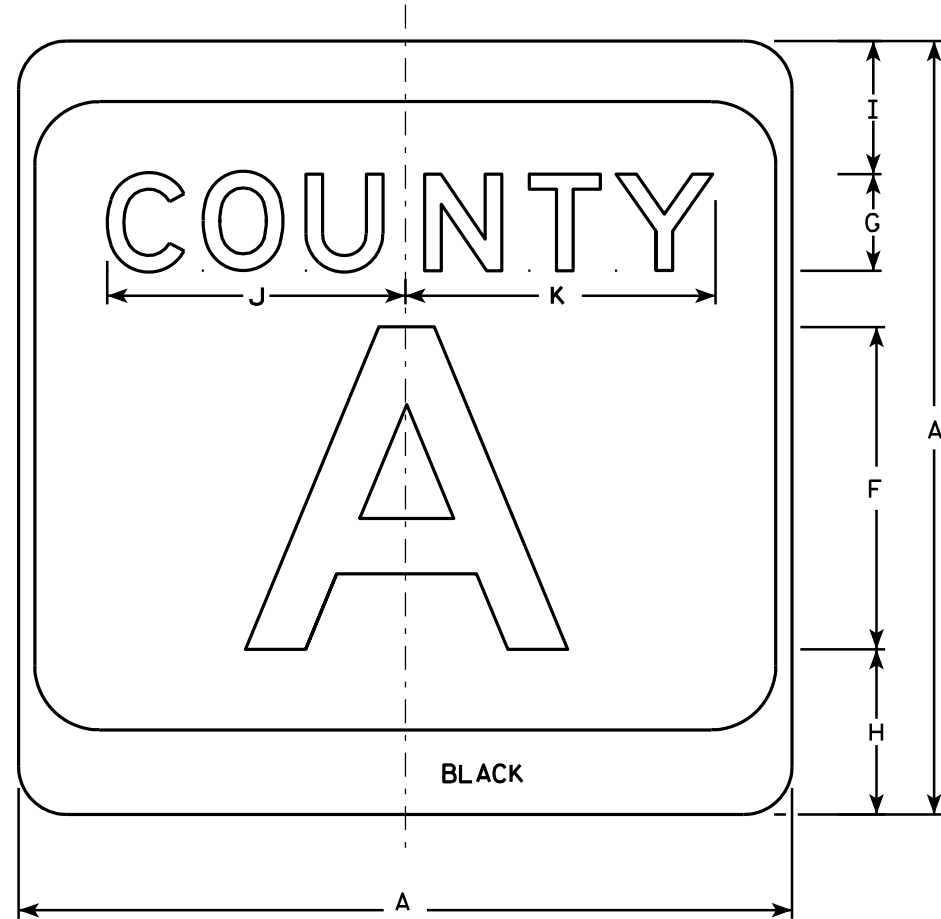
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Rauch*  
for State Traffic Engineer

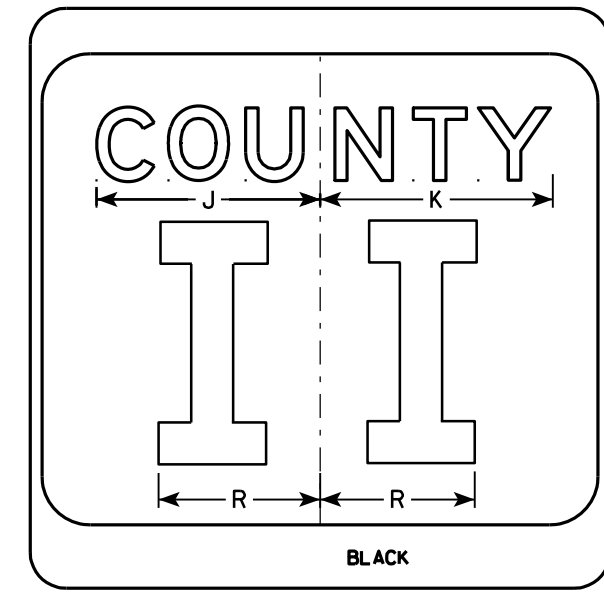
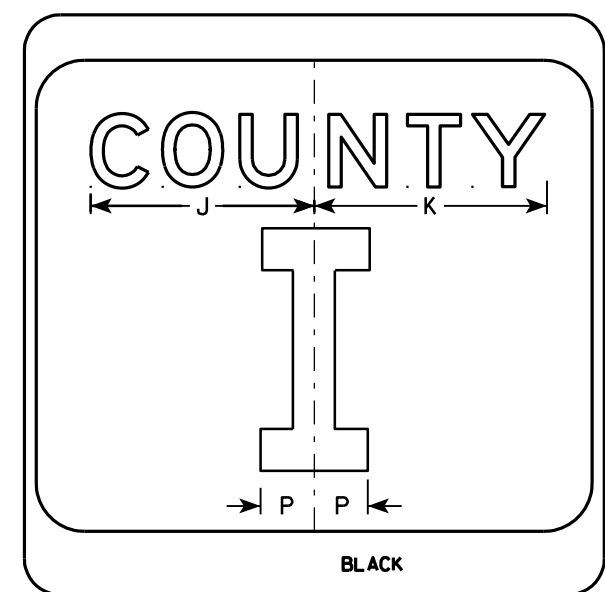
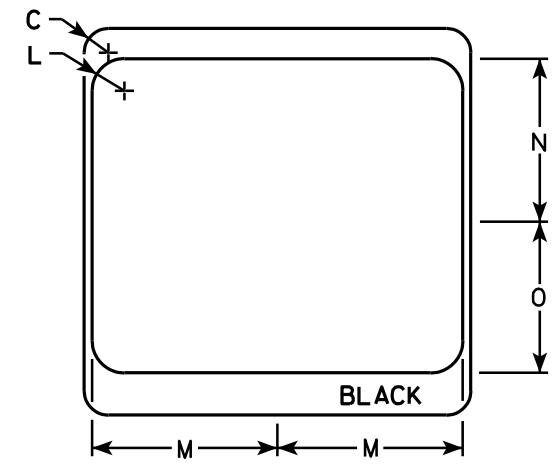
DATE 3/25/19 PLATE NO. D11-10.5

**NOTES**

1. Sign is Type II - see Note 7 - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - White & Black - See Note 7  
Message - Black
3. Message Series - see Note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Message Series E for 1 letter.  
Message Series D for 2 letters unless message is too big then Series C.  
Message Series C for 3 letters unless message is too big then Series B.
6. Substitute appropriate letters & optically center to achieve proper balance.
7. Permanent Signs  
Background - Type H Reflective  
Detour or temporary Signs  
Background - Reflective



M1-5A



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24		1 1/2			10	3	5 1/8	4 1/8	9 1/4	9 5/8	2	11 1/2	10 1/8	9 3/8	2 1/4		6 5/8									4.0
3	36		2 1/4			16	4	7 5/8	5 5/8	12 1/4	12 7/8	3	17 1/8	15 1/4	14	3 3/8		10									9.0
4	36		2 1/4			16	4	7 5/8	5 5/8	12 1/4	12 7/8	3	17 1/8	15 1/4	14	3 3/8		10									9.0
5	36		2 1/4			16	4	7 5/8	5 5/8	12 1/4	12 7/8	3	17 1/8	15 1/4	14	3 3/8		10									9.0

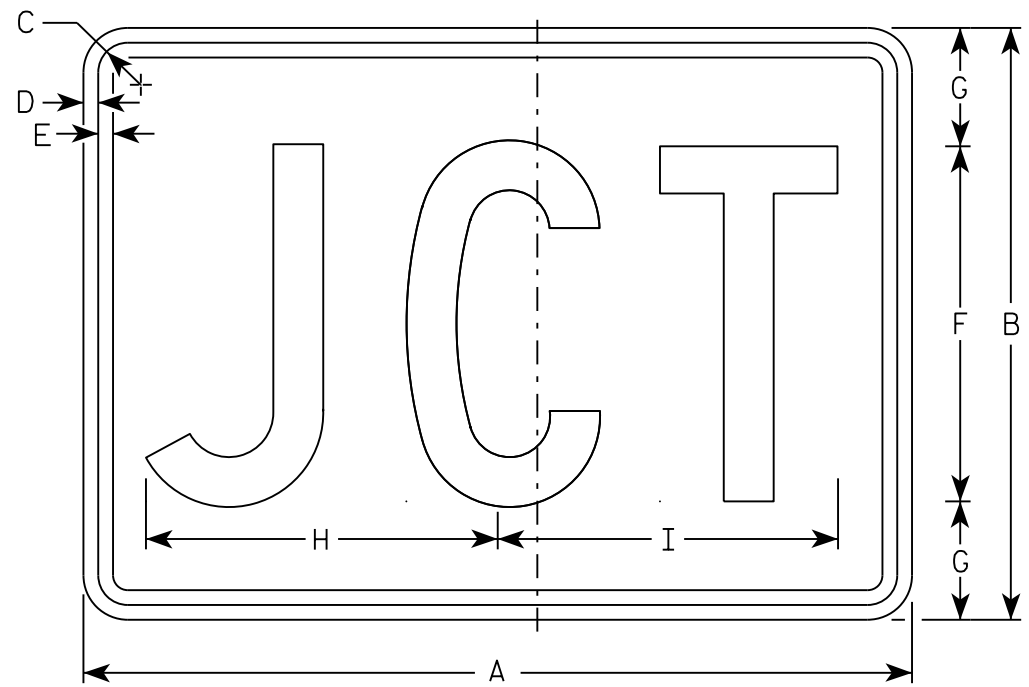
**CTH MARKER**  
**M1-5A FOR ASSEMBLIES**

WISCONSIN DEPT OF TRANSPORTATION

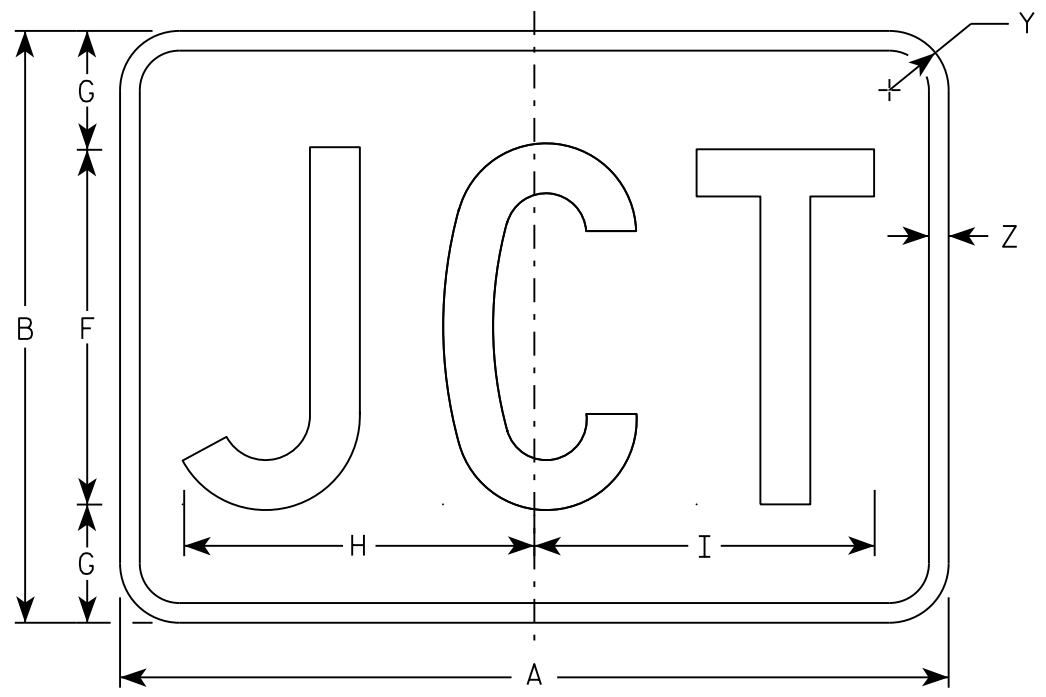
APPROVED *Matthew R. Raub*  
For State Traffic Engineer

DATE 9/27/11 PLATE NO. MI-5A.8

PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: **E**



M2-1  
MM2-1  
MP2-1



MB2-1  
MK2-1  
MN2-1  
MR2-1

NOTES

1. Sign is Type II - Type H
2. Color:  
Background - See note 5  
Message - See note 5
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. M2-1 Background - White  
Message - Black  
MB2-1 Background - Blue  
Message - White  
MK2-1 Background - Green  
Message - White  
MM2-1 Background - White  
Message - Green  
MN2-1 Background - Brown  
Message - White  
MP2-1 Background - White  
Message - Blue  
MR2-1 Background - Brown  
Message - Yellow

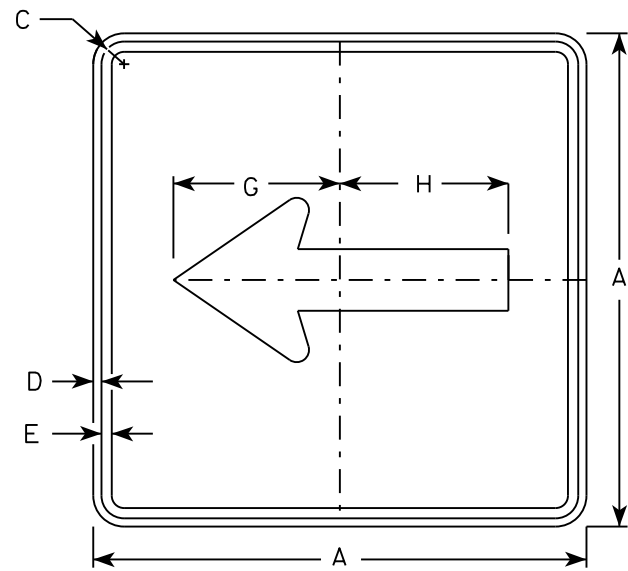
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21	15	1 1/8	3/8	3/8	9	3	8 7/8	8 5/8																1 1/2	1/2	2.20
3	30	21	1 1/8	3/8	3/8	13	4	12 7/8	12 3/8																1 1/2	1/2	4.40
4	30	21	1 1/8	3/8	3/8	13	4	12 7/8	12 3/8																1 1/2	1/2	4.40
5	30	21	1 1/8	3/8	3/8	13	4	12 7/8	12 3/8																1 1/2	1/2	4.40

STANDARD SIGN  
M2-1

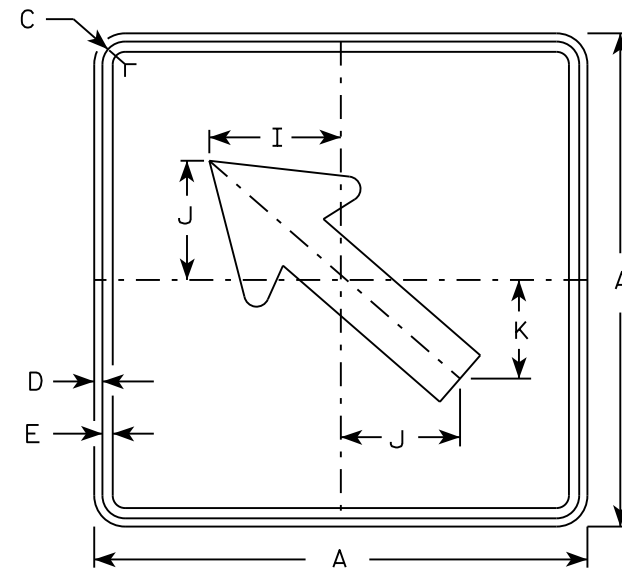
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
For State Traffic Engineer

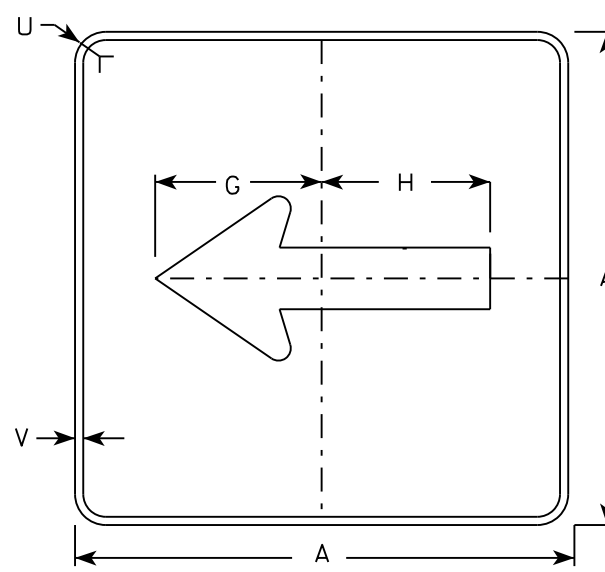
DATE 10/15/15 PLATE NO. M2-1.12



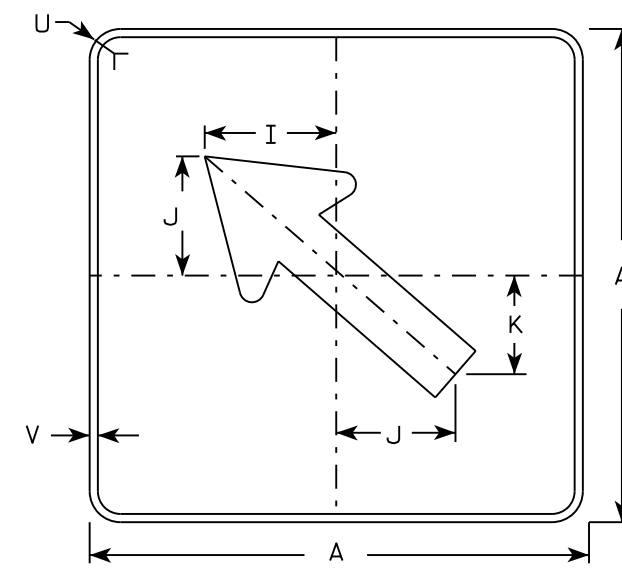
M6-1  
MM6-1  
M06-1  
MP6-1



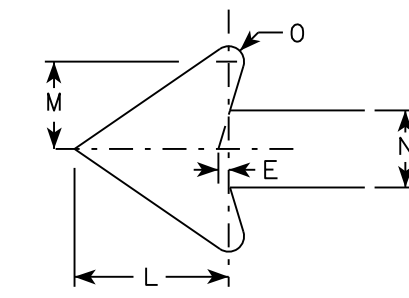
M6-2  
MM6-2  
M06-2  
MP6-2



MB6-1  
MK6-1  
MN6-1  
MR6-1



MB6-2  
MK6-2  
MN6-2  
MR6-2



NOTES

- Signs are Type II - Type H except as Shown
- Color:  
Background - See note 4  
Message - See note 4
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- M6-1 and M6-2 Background - White  
Message - Black  
MB6-1 and MB6-2 Background - Blue  
Message - White  
MK6-1 and MK6-2 Background - Green  
Message - White  
MM6-1 and MM6-2 Background - White  
Message - Green  
MN6-1 and MN6-2 Background - Brown  
Message - White  
M06-1 and M06-2 Background - Orange - Type F Reflective  
Message - Black  
MP6-1 and MP6-2 Background - White  
Message - Blue  
MR6-1 and MR6-2 Background - Brown  
Message - Yellow

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7 1/2	7 1/8	5 5/8	5	4 1/4	5 1/4	3	2 5/8	1/2						1 1/2	1/2					3.06
3	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25
4	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25
5	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25

STANDARD SIGN  
M6-1 & M6-2  
SERIES

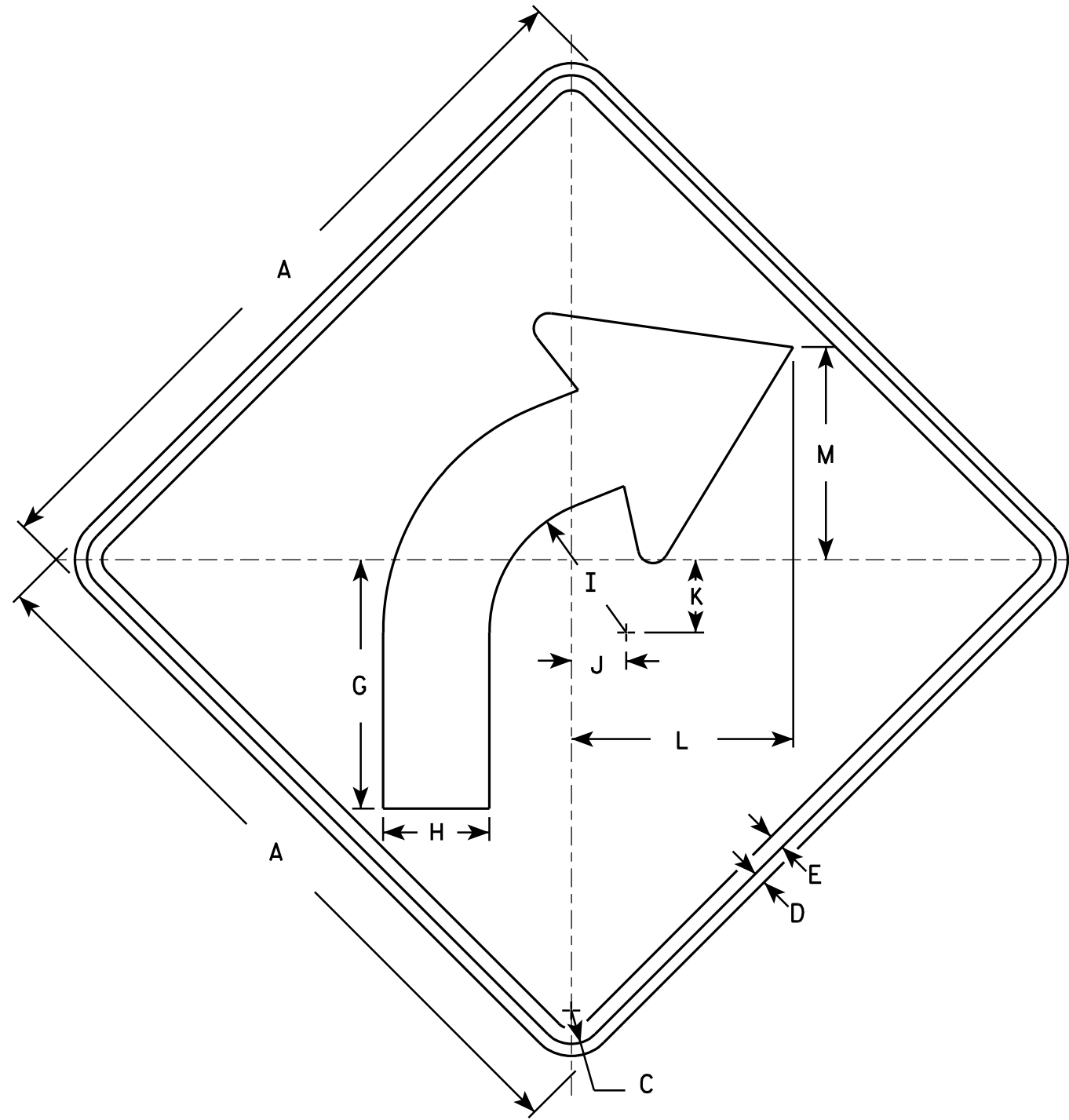
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

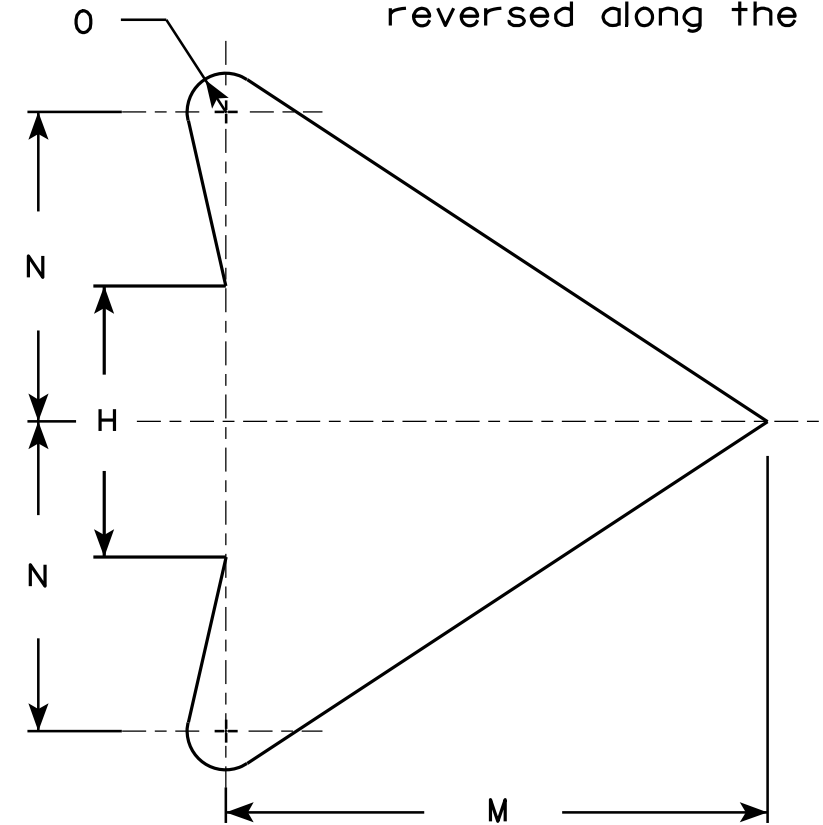
DATE 10/15/15 PLATE NO. M6-1.15

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - Yellow  
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. W1-2L is the same as W1-2R except the arrow is reversed along the vertical centerline.



W1-2R



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2		8 1/4	3 1/2	4 1/2	1 3/4	2 3/8	7 1/4	7	4	1/2												4.0
2S	30		1 3/8	1/2	5/8		10 1/4	4 3/8	5 5/8	2 1/4	3	9 1/8	8 3/4	5	5/8												6.25
2M	36		1 5/8	5/8	3/4		12 3/8	5 1/4	6 3/4	2 5/8	3 1/2	10 7/8	10 1/2	6	3/4												9.0
3	36		1 5/8	5/8	3/4		12 3/8	5 1/4	6 3/4	2 5/8	3 1/2	10 7/8	10 1/2	6	3/4												9.0
4	36		1 5/8	5/8	3/4		12 3/8	5 1/4	6 3/4	2 5/8	3 1/2	10 7/8	10 1/2	6	3/4												9.0
5	48		2 1/4	3/4	1		16 1/2	7	9	3 1/2	4 5/8	14 1/2	14	8	1												16.0

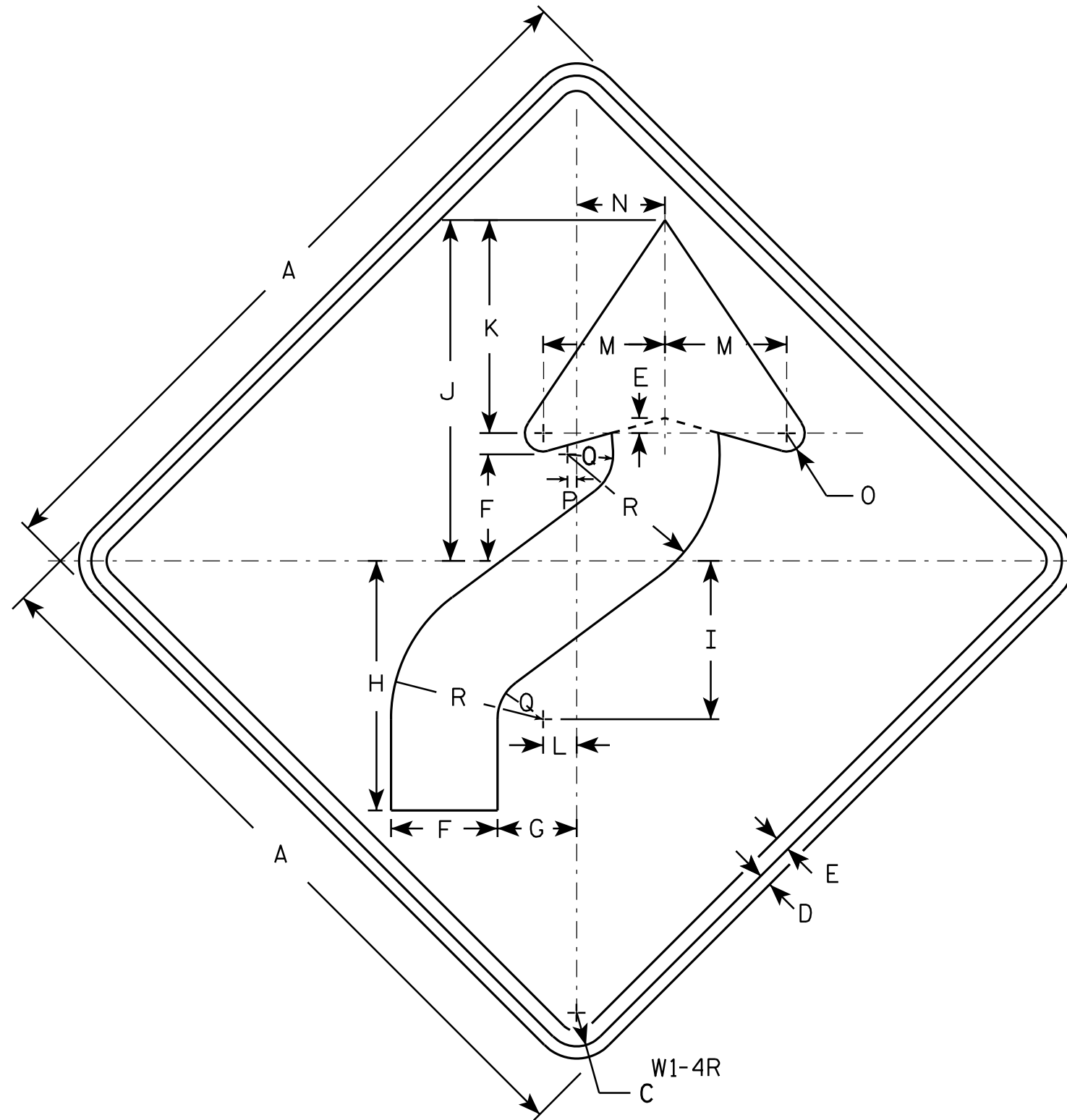
STANDARD SIGN  
W1-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
For State Traffic Engineer

DATE 5/15/12 PLATE NO. W1-2.10





NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - Yellow  
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. W1-4L is the same as W1-4R except the arrow is reversed along the vertical centerline.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2	3 1/2	2 5/8	8 1/4	5 1/4	11 1/4	7	1 1/8	4	3	5/8	1/4	1 1/2	5									4.0
2S	30		1 3/8	1/2	5/8	4 3/8	3 1/4	10 1/4	6 1/2	14	8 3/4	1 3/8	5	3 5/8	3/4	3/8	1 7/8	6 1/4									6.25
2M	36		1 5/8	5/8	3/4	5 1/4	4	12 3/8	7 7/8	16 7/8	10 1/2	1 5/8	6	4 1/2	1	1/2	2 1/4	7 1/2									9.0
3	36		1 5/8	5/8	3/4	5 1/4	4	12 3/8	7 7/8	16 7/8	10 1/2	1 5/8	6	4 1/2	1	1/2	2 1/4	7 1/2									9.0
4	36		1 5/8	5/8	3/4	5 1/4	4	12 3/8	7 7/8	16 7/8	10 1/2	1 5/8	6	4 1/2	1	1/2	2 1/4	7 1/2									9.0
5	48		2 1/4	3/4	1	7	5 1/4	16 1/2	10 1/2	22 1/2	14	2 1/4	8	6	1 1/4	5/8	3	10									16.0

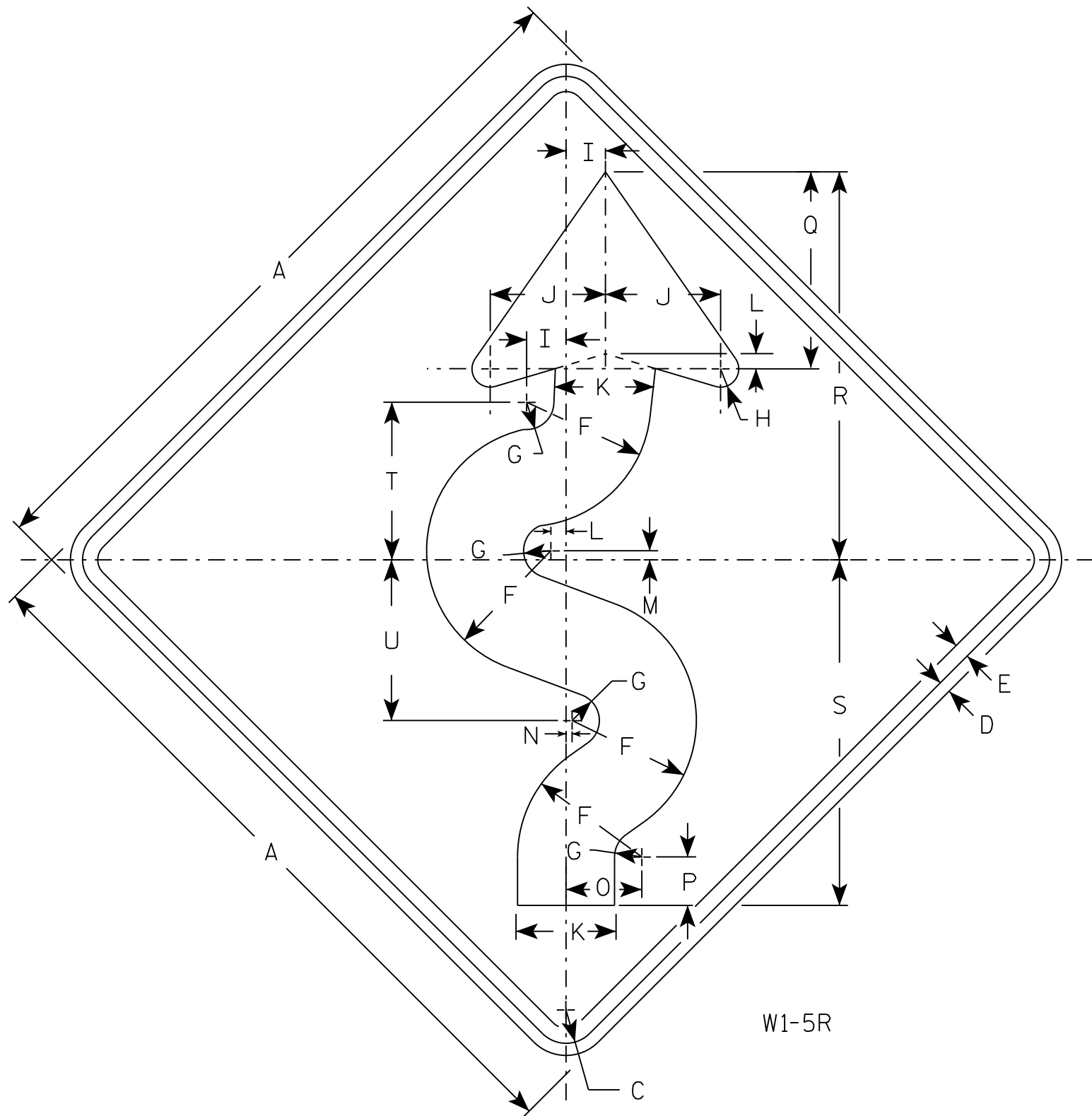
STANDARD SIGN  
W1-4

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Raub*  
for State Traffic Engineer

DATE 5/17/12 PLATE NO. W1-4.11

PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: \_\_\_\_\_ E



NOTES

1. Sign is Type II - Type F Reflective
2. Color:  
Background - Yellow  
Message - Black
3. W1-5L is the same as W1-5R except the arrow is reversed along the vertical centerline.
4. If used with W13-1 of 30 MPH or less, use 36" sign for Size 2S.

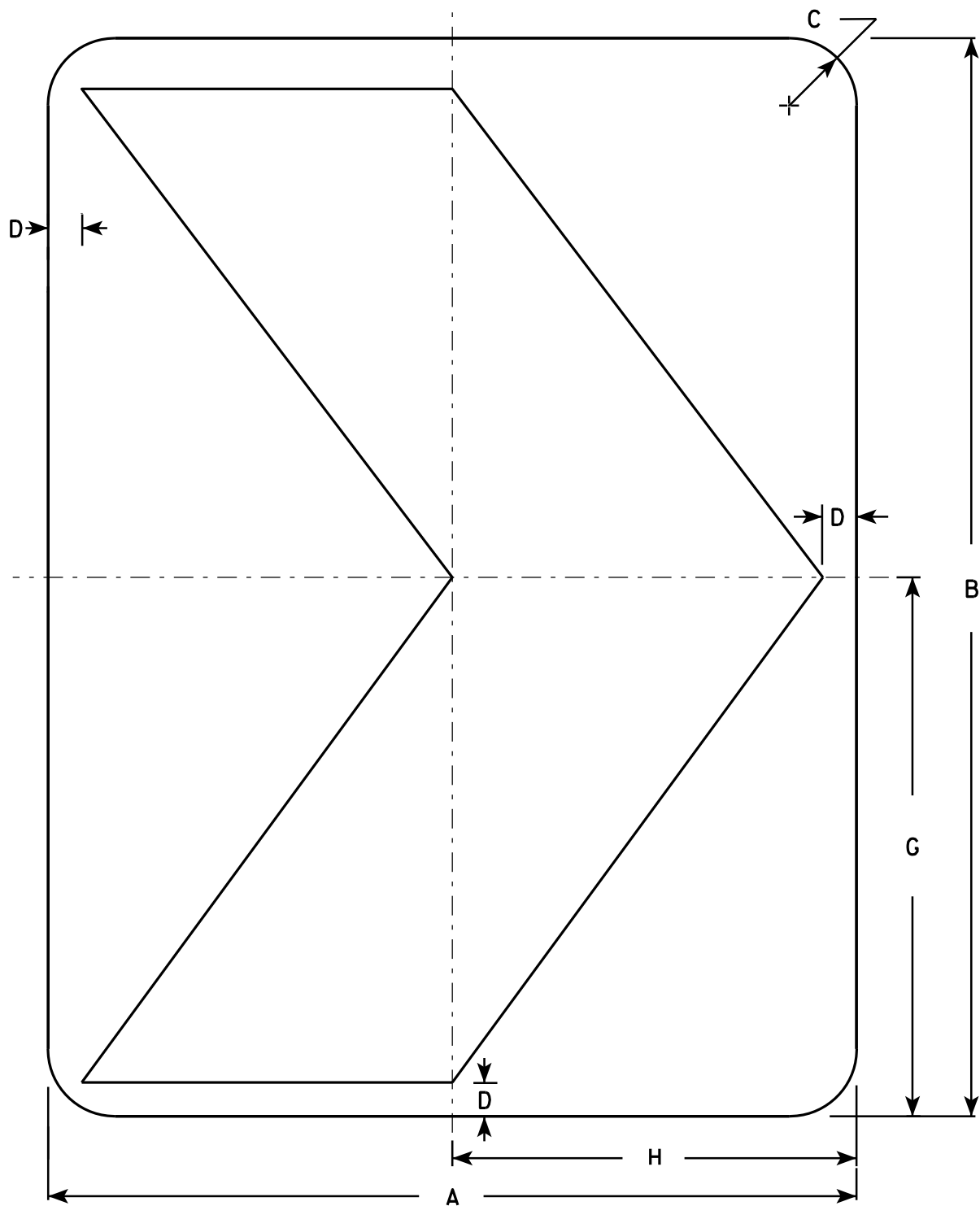
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2	4 1/8	7/8	5/8	1 1/4	3 3/4	3 1/4	1/2	1/4	1/8	2 1/2	1 5/8	6 1/2	12 3/4	11 3/8	5 1/4	5 1/4						4.0
2S	30		1 3/8	1/2	5/8	5 1/8	1 1/8	3/4	1 5/8	4 3/4	4 1/8	5/8	3/8	1/4	3 1/8	2	8 1/8	16	14 1/4	6 1/2	6 5/8						6.25
2M	36		1 5/8	5/8	3/4	6 1/4	1 3/8	1	1 7/8	5 5/8	4 7/8	3/4	3/8	1/4	3 3/4	2 7/8	9 3/4	19 1/8	17 1/8	7 3/4	7 7/8						9.0
3	36		1 5/8	5/8	3/4	6 1/4	1 3/8	1	1 7/8	5 5/8	4 7/8	3/4	3/8	1/4	3 3/4	2 7/8	9 3/4	19 1/8	17 1/8	7 3/4	7 7/8						9.0
4	36		1 5/8	5/8	3/4	6 1/4	1 3/8	1	1 7/8	5 5/8	4 7/8	3/4	3/8	1/4	3 3/4	2 7/8	9 3/4	19 1/8	17 1/8	7 3/4	7 7/8						9.0
5	48		2 1/4	3/4	1	8 1/4	1 3/4	1 1/4	2 1/2	7 1/2	6 1/2	1	1/2	3/8	5	3 1/4	13	25 1/2	22 3/4	10 3/8	10 1/2						16.0

STANDARD SIGN  
W1-5

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 8/1/16 PLATE NO. W1-5.9



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - Yellow  
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

7

7

W1-8

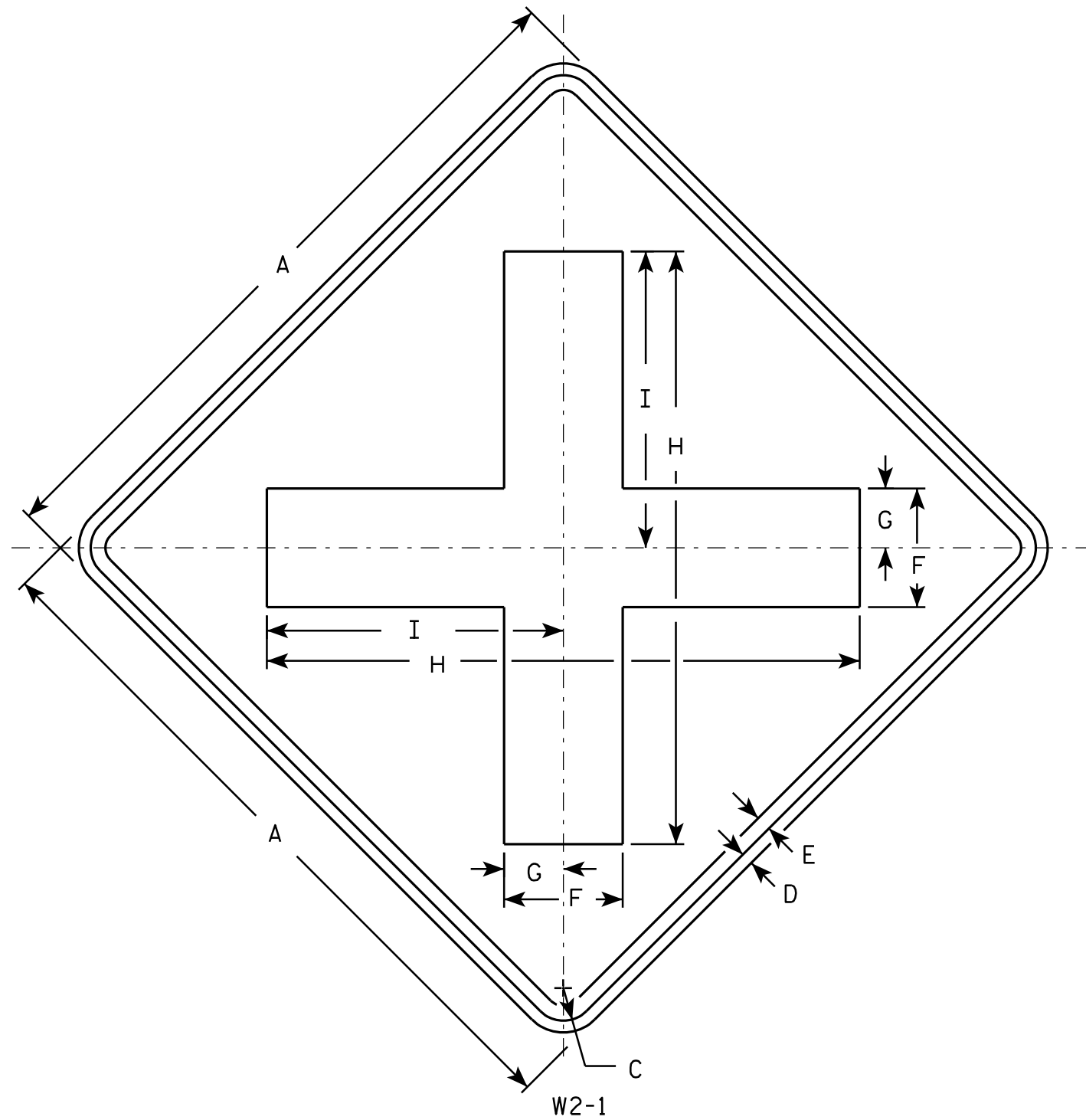
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	12	18	1 1/2	1/2			9	6																			1.5
2S	18	24	1 1/2	3/4			12	9																			3.0
2M	18	24	1 1/2	3/4			12	9																			3.0
3	24	30	1 1/2	1			15	12																			5.0
4	30	36	1 7/8	1 1/4			18	15																			7.5
5	36	48	2 1/4	1 1/2			24	18																			12.0

**STANDARD SIGN**  
W1-8

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 6/7/10 PLATE NO. W1-8.6



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - Yellow  
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2	4	2	20	10																		4.0
2S	30		1 3/8	1/2	5/8	5	2 1/2	25	12 1/2																		6.25
2M	30		1 3/8	1/2	5/8	5	2 1/2	25	12 1/2																		6.25
3	36		1 5/8	5/8	3/4	6	3	30	15																		9.0
4	48		2 1/4	3/4	1	8	4	40	20																		16.0
5																											

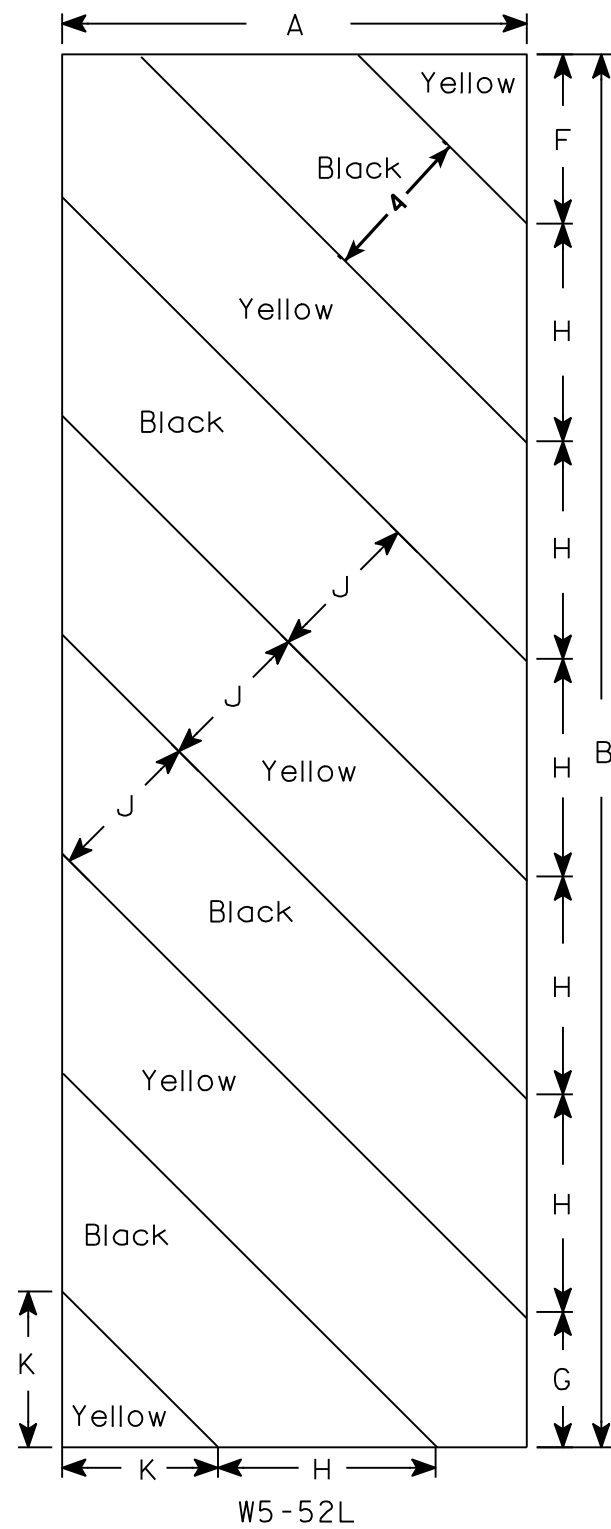
**STANDARD SIGN**  
**W2-1**

*WISCONSIN DEPT OF TRANSPORTATION*

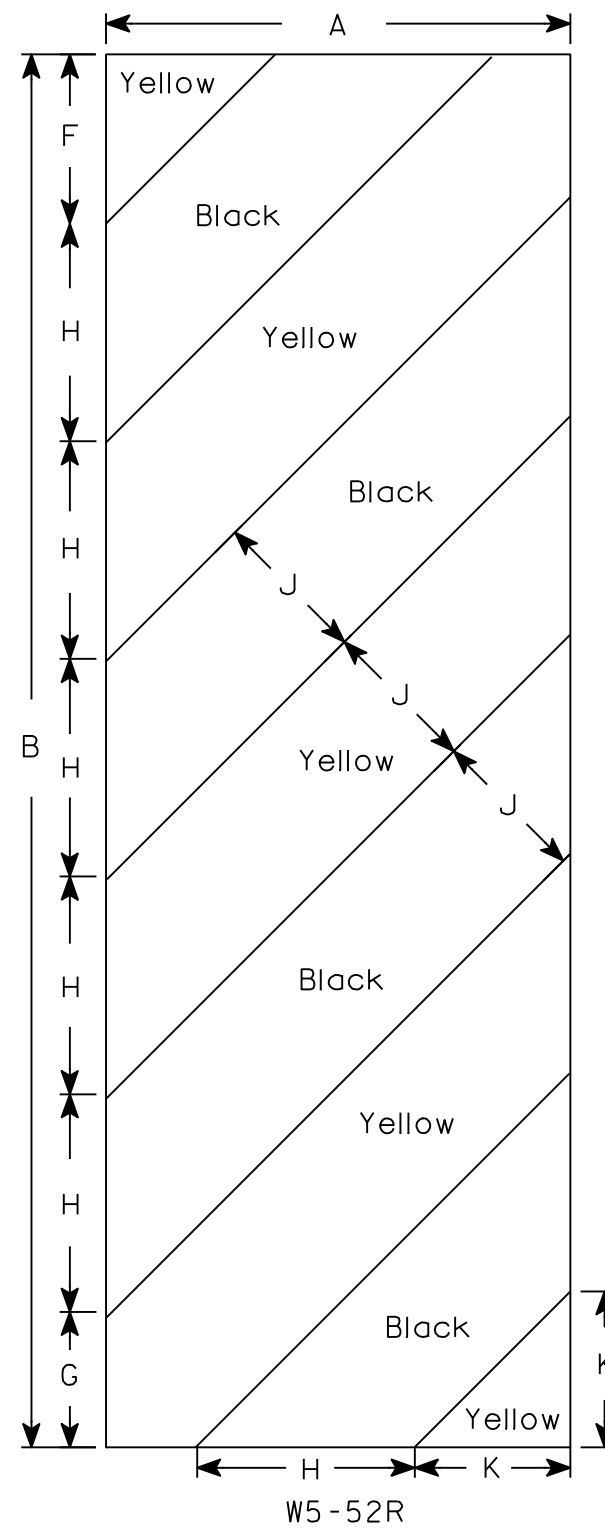
APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 5/29/12 PLATE NO. W2-1.9

PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: \_\_\_\_\_ E



W5-52L



W5-52R

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - Yellow  
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. Alternate colors of stripes as shown.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	12	36				4 3/8	3 1/2	5 5/8	45°	4	4																3.0
2M	12	36				4 3/8	3 1/2	5 5/8	45°	4	4																3.0
3	18	54				6	5 1/2	8 1/2	45°	6	6 9/16																6.75
4																											
5																											

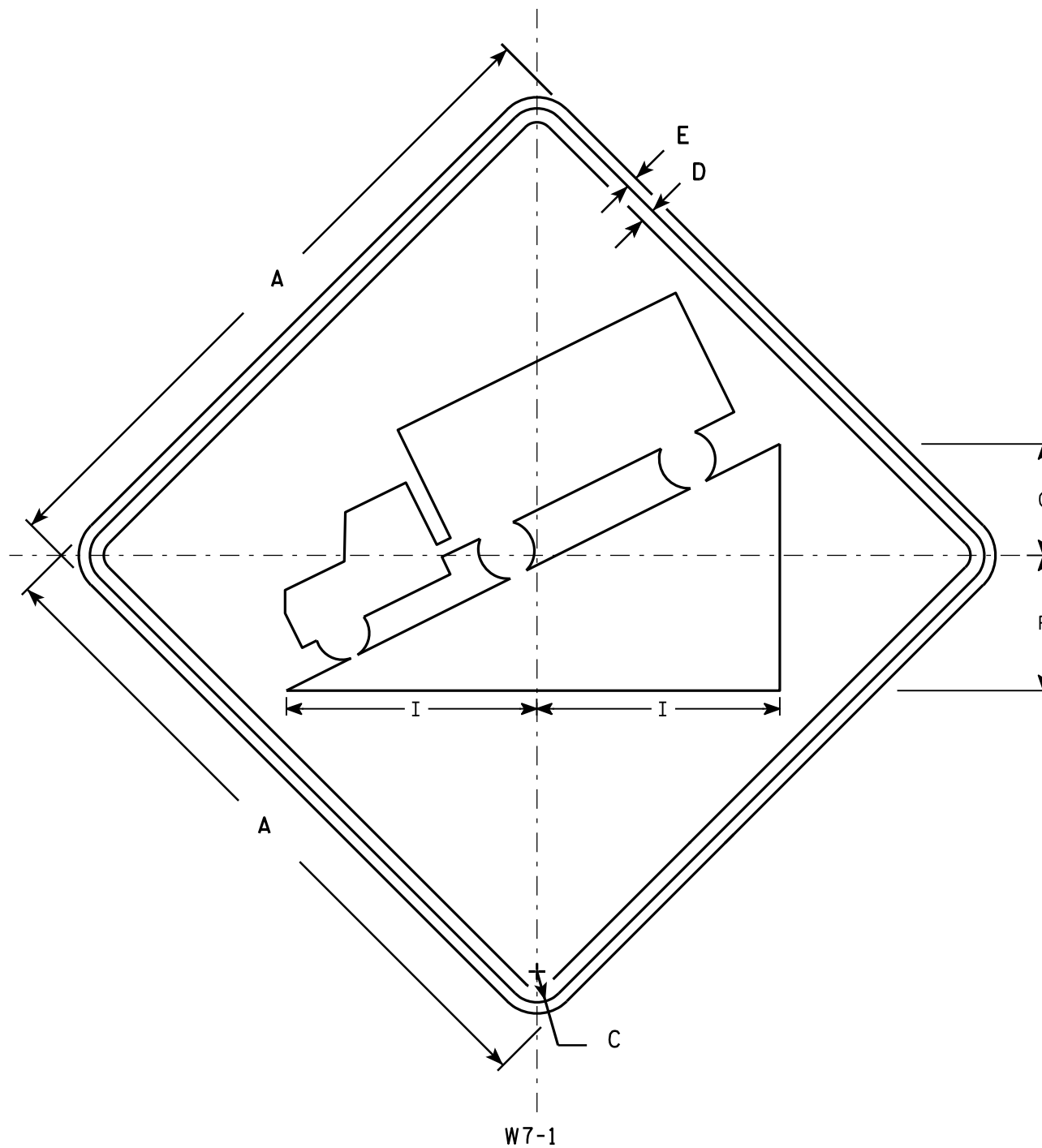
STANDARD SIGN  
W5-52L & W5-52R

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 5/29/12 PLATE NO. W5-52.9

PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: \_\_\_\_\_ E



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - Yellow  
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	3/8	5	4		8 3/4																		4.0
2S	30		1 3/8	1/2	5/8	6	5		11																		6.25
2M	36		1 5/8	5/8	3/4	7 1/4	6		13 1/4																		9.0
3	36		1 5/8	5/8	3/4	7 1/4	6		13 1/4																		9.0
4	36		1 5/8	5/8	3/4	7 1/4	6		13 1/4																		9.0
5	48		2 1/4	3/4	1	9 3/4	8		17 1/2																		16.0

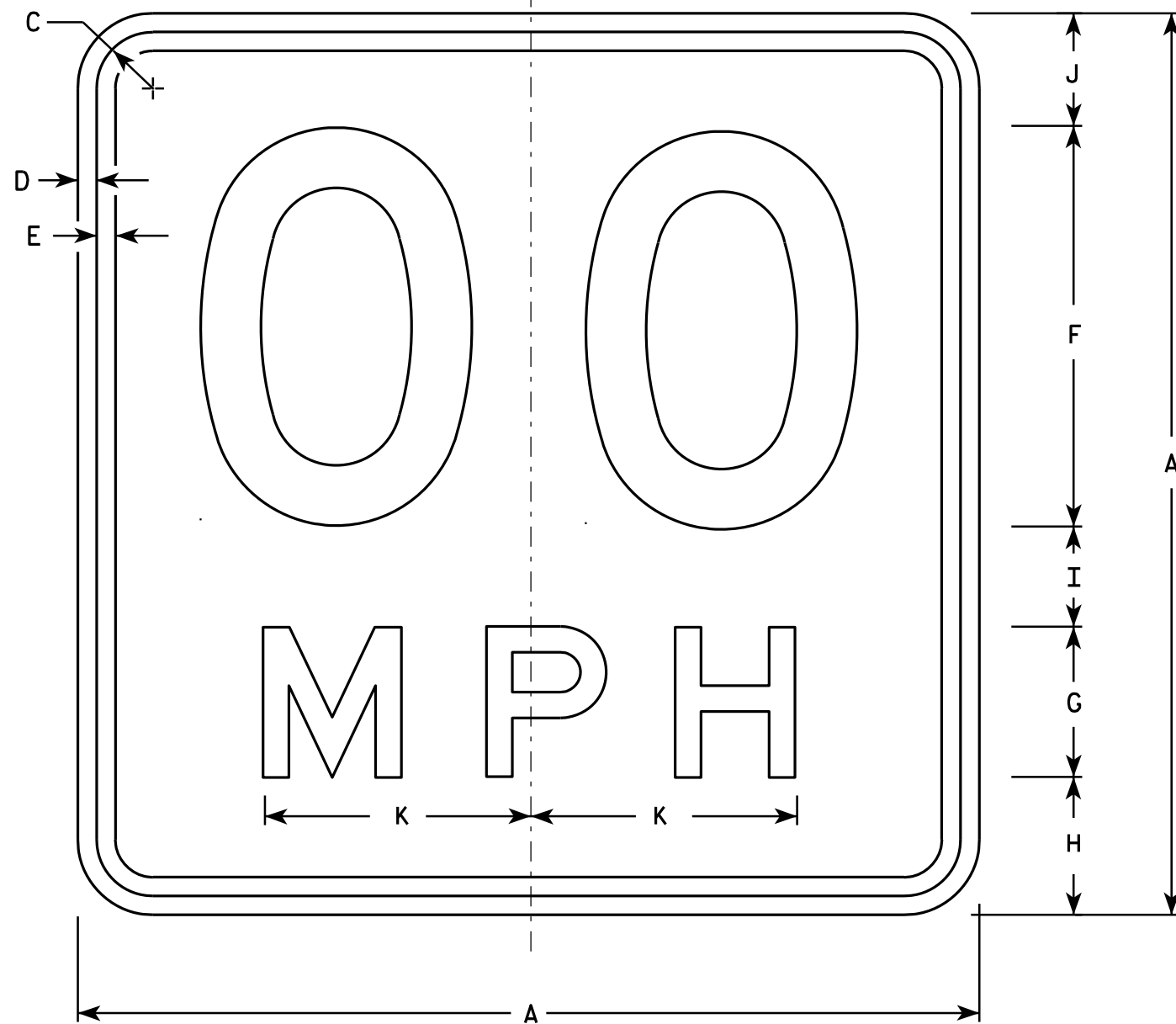
**STANDARD SIGN**  
**W7-1**

*WISCONSIN DEPT OF TRANSPORTATION*

APPROVED *Matthew R. Rauch*  
For State Traffic Engineer

DATE 03/12/13 PLATE NO. W7-1.13

PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: **E**



**NOTES**

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:  
Background - Yellow  
Message - Black
3. Message Series - See Note 6
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals and optically space about centerline to achieve proper balance.
6. Line 1 is Series D  
Line 2 is Series E

W13-1

\* For 30" x 30" Warning Signs, use 18" x 18" W13-1 signs.  
For 36" x 36" Warning Signs, use 24" x 24" W13-1 signs.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18		1 1/8	3/8	3/8	8	3	2 3/4	2	2 1/4	5 3/8															2.25	
* 2S	18		1 1/8	3/8	3/8	8	3	2 3/4	2	2 1/4	5 3/8															2.25	
* 2M	18		1 1/8	3/8	3/8	8	3	2 3/4	2	2 1/4	5 3/8															2.25	
3	24		1 1/8	3/8	1/2	10	4	4	2 3/4	3 1/4	6 5/8															4.00	
4	36		1 5/8	5/8	3/4	16	6	5 1/2	4	4 1/2	10 5/8															9.00	
5	36		1 5/8	5/8	3/4	16	6	5 1/2	4	4 1/2	10 5/8															9.00	

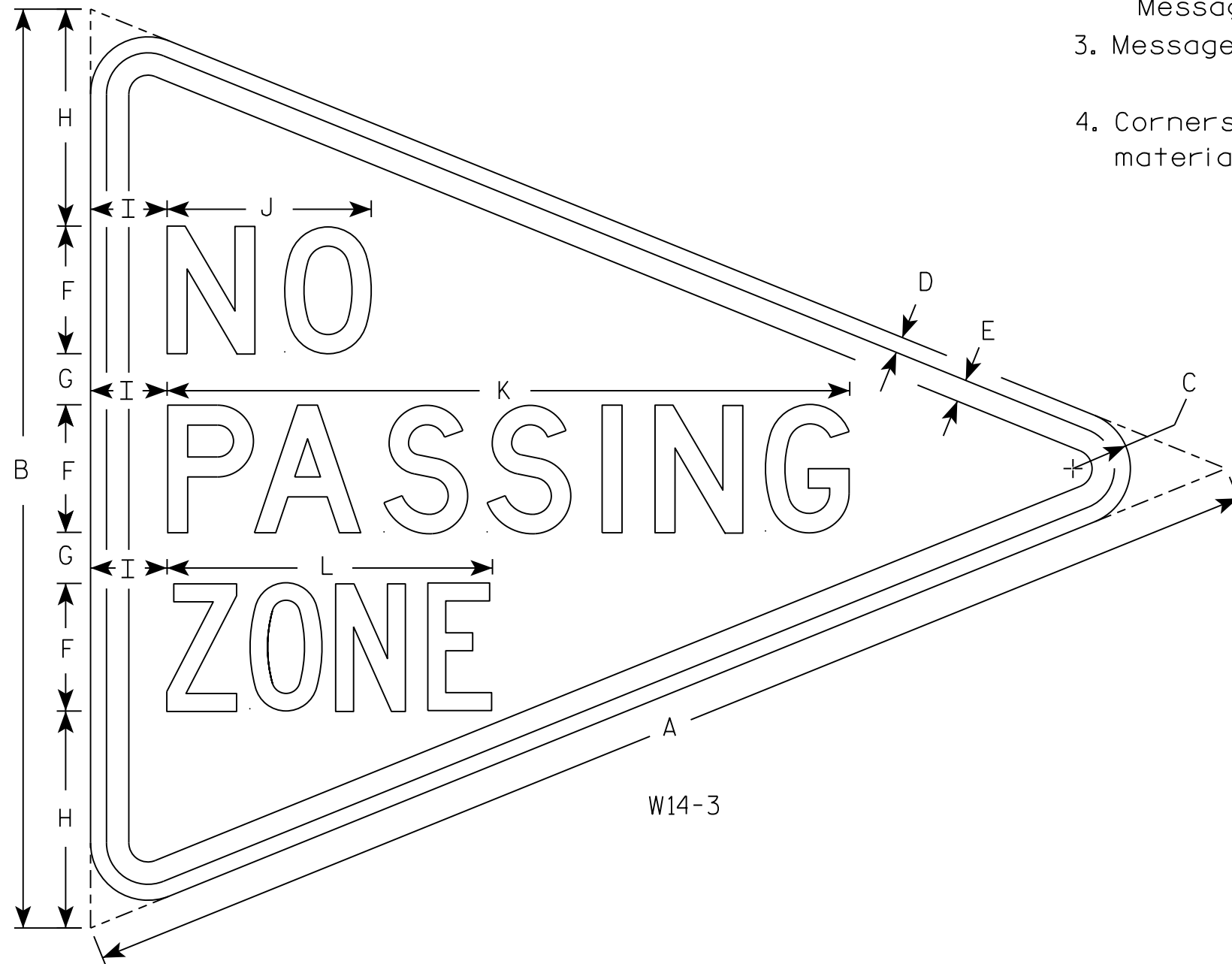
STANDARD SIGN  
W13-1

WISCONSIN DEPT OF TRANSPORTATION  
APPROVED *Matthew R. Rauch*  
For State Traffic Engineer

DATE 5/31/12 PLATE NO. W13-1.16

NOTES

1. Sign is Type II - Type F Reflective
2. Color:  
Background - Yellow  
Message - Black
3. Message Series - Lines 1 and 2 are Series D.  
Line 3 is series C.
4. Corners and borders shall be rounded on all base materials for this sign.



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48	36	2 1/4	5/8	7/8	5	2	8 1/2	3	8	26 3/4	12 3/4															5.56
2M																											
3																											
4																											
5																											

STANDARD SIGN  
W14-3

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*  
for State Traffic Engineer

DATE 3/21/17 PLATE NO. W14-3.10

PROJECT NO: \_\_\_\_\_ HWY: \_\_\_\_\_ COUNTY: \_\_\_\_\_ SHEET NO: **E**



# Notes



## ***Wisconsin Department of Transportation***

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