

WKE

Nov 8, 2022

PROJECT ID:

2707-03-74

COUNTY:

WASHINGTON COUNTY

ORDER OF SHEETS

| | | |
|------------------------|--------------|------------------------------------|
| Section No. | 1 | Title |
| Section No. | 2 | Typical Sections and Details |
| Section No. | 3 | Estimate of Quantities |
| Section No. | 3 | Miscellaneous Quantities |
| Section No. | 4 | Right of Way Plat |
| Section No. | 5 | Plan and Profile |
| Section No. | 6 | Standard Detail Drawings |
| Section No. | 7 | Sign Plates |
| Section No. | 8 | Structure Plans |
| Section No. | 9 | Computer Earthwork Data |
| Section No. | 9 | Cross Sections |

TOTAL SHEETS = 48



29

DESIGN DESIGNATION

| | |
|--------------|----|
| A.A.D.T. | == |
| A.A.D.T. | == |
| D.H.V. | == |
| D.D. | == |
| T. | == |
| DESIGN SPEED | == |
| ESALS | == |

CONVENTIONAL SYMBOLS

| | |
|--------------------------------|---------|
| PLAN | |
| CORPORATE LIMITS | //// |
| PROPERTY LINE | --- |
| LOT LINE | - - - - |
| LIMITED HIGHWAY EASEMENT | - - - - |
| EXISTING RIGHT OF WAY | --- |
| PROPOSED OR NEW R/W LINE | --- |
| SLOPE INTERCEPT | - - - - |
| REFERENCE LINE | --- |
| EXISTING CULVERT | --- |
| PROPOSED CULVERT (Box or Pipe) | --- |
| COMBUSTIBLE FLUIDS | CAUTION |
| MARSH AREA | ▲▲▲ |
| WOODED OR SHRUB AREA | ▨ |

| | |
|---|-------|
| PROFILE | |
| GRADE LINE | --- |
| ORIGINAL GROUND | --- |
| MARSH OR ROCK PROFILE (To be noted as such) | --- |
| SPECIAL DITCH | --- |
| GRADE ELEVATION | 55.36 |
| CULVERT (Profile View) | ○ □ |
| UTILITIES | |
| ELECTRIC | --- |
| FIBER OPTIC | --- |
| GAS | --- |
| SANITARY SEWER | --- |
| STORM SEWER | --- |
| TELEPHONE | --- |
| WATER | --- |
| UTILITY PEDESTAL | --- |
| POWER POLE | --- |
| TELEPHONE POLE | --- |

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

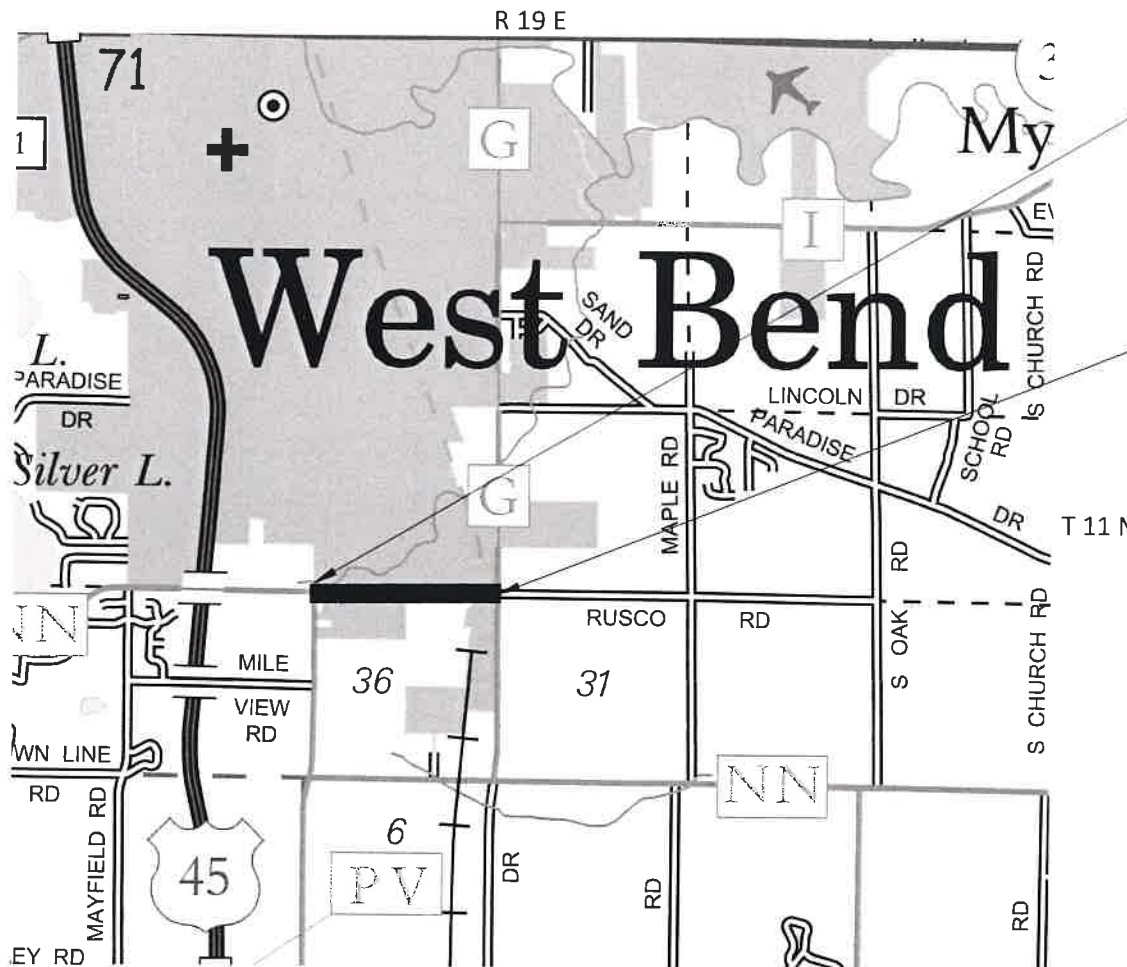
C WEST BEND, RUSCO DR

S MAIN ST TO RIVER ROAD

LOCAL STREET

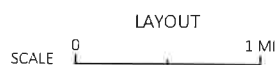
WASHINGTON COUNTY

STATE PROJECT NUMBER
2707-03-74



BEGIN PROJECT
STA: 1+20.63
Y=169,575.57
X=362,946.55

END PROJECT
STA: 53+66.95
Y=169,552.90
X=368,192.55



TOTAL NET LENGTH OF CENTERLINE = 5246 FEET

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS), WASHINGTON COUNTY, NSRS2011, IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES.

ELEVATIONS ARE REFERENCED TO NAD 83.

GPS DERIVED ELEVATIONS ARE BASED ON GEOID 12A.

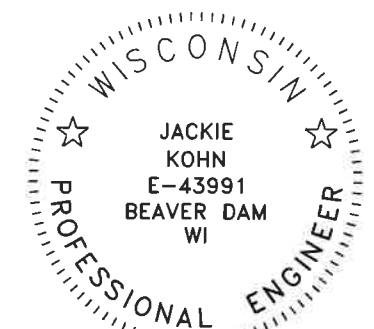
| | | |
|---------------|-----------------|----------|
| STATE PROJECT | FEDERAL PROJECT | |
| | PROJECT | CONTRACT |
| 2707-03-74 | WISC 2022030 | 1 |
| | | |
| | | |

ACCEPTED FOR

CITY OF WEST BEND

Date: Sept 07, 2022
Jackie Kohn-Born
City Engineer
(Signature and Title of Official)

ORIGINAL PLANS PREPARED BY
KUNKEL ENGINEERING GROUP



DATE: 07/29/2022
J. Kohn-Born
(Professional Engineer Signature)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

| | |
|---------------------|--------------------------------|
| PREPARED BY | |
| Surveyor | KUNKEL ENGINEERING GROUP (KEG) |
| Designer | JACKIE KOHN-BORN, PE (KEG) |
| Project Manager | MITCHELL LEISSES (KEG) |
| Regional Examiner | REGIONAL EXAMINER |
| Regional Supervisor | REGIONAL SUPERVISOR |

APPROVED FOR THE DEPARTMENT
DATE: 9/7/2022
Michael J. Baird
(Signature)

E

UTILITIES CONTACTS

***City of West Bend**
 Engineering Department
 1115 South Main Street
 West Bend, WI 53095
 (262) 335-5130
cityeng@ci.west-bend.wi.us

***AT&T**
 435 S. 95th Street
 Milwaukee, WI, 53214
 Mr. Dean Herro
 (262) 226-9639
dh2572@att.com

***Charter / Spectrum**
 1515 W. Washington Street
 West Bend, WI 53095
 Mr. Nick Frase
 (920) 304-6797
nick.frase@charter.com

***Water Utility**
 Mr. Travis Thull
 Water Utility Manager
 (262) 335-5040
thullt@ci.west-bend.wi.us

***Sprint / T-Mobile**
 849 Earl Street
 Saint Paul, MN 55106
 Mr. Dan Hilliard
 (612) 217-3526
dan.j.hilliard@t-mobile.com

***We Energies - Electric & Gas**
 500 S. 116th Street
 West Allis, WI 53214
 Ms. Alicia Holdorf
 (414) 944-5695
alicia.holdorf@we-energies.com



GENERAL NOTES

THE LOCATIONS OF EXISTING UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN. THE CONTRACTOR SHALL COORDINATE THEIR CONSTRUCTION ACTIVITIES WITH A CALL TO DIGGERS HOTLINE AND/OR A DIRECT CALL TO THE UTILITIES THAT HAVE FACILITIES IN THE AREA. NOT ALL UTILITIES ARE MEMBERS OF DIGGERS HOTLINE.

THE CONTRACTOR IS TO WORK WITH UTMOST CARE AND PROTECT ALL SURVEY MARKERS. REMOVAL OF ANY SURVEY MARKER IS TO BE WITH THE APPROVAL OF THE ENGINEER.

RADIUS DIMENSIONS FOR THE CURB AND GUTTER ARE TO THE BACK OF CURB, UNLESS OTHERWISE NOTED. ALL OTHER RADII ARE MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE SHOWN OR NOTED ON THE PLAN.

CURB AND GUTTER PLAN GRADES ARE TO THE TOP OF CURB LINE UNLESS OTHERWISE NOTED.

EXPANSION JOINTS TO BE CONSTRUCTED AT ALL RADIUS POINTS IN CURB AND GUTTER OR AT LOCATIONS SHOWN ON THE PLAN. CONSTRUCT INSIDE EDGE OF SIDEWALK 1/2 INCH HIGHER THAN TOP OF CURB WHEN THEY ARE ADJACENT TO EACH OTHER.

SAWCUTS, AS SHOWN ON PLANS, ARE AT SUGGESTED LOCATIONS AND MAY BE ADJUSTED AT THE DISCRETION OF THE ENGINEER TO BETTER SUIT FIELD CONDITIONS.

PAVEMENT REMOVAL WILL BE TO THE NEAREST JOINT OR A SAWED EDGE WILL BE REQUIRED AS DIRECTED BY THE ENGINEER.

EXISTING DRAINAGE FACILITIES WILL REMAIN FUNCTIONAL DURING EXCAVATION OPERATIONS.

ASPHALTIC COMMERCIAL AND PRIVATE ENTRANCES SHALL BE SAWCUT AS REQUIRED TO PROVIDE A CLEAN BUTT JOINT TO NEW WORK.

WHEREVER ASPHALTIC CONCRETE PAVEMENT ABUTS OR OVERLAPS ONTO PORTLAND CEMENT CONCRETE, THE EXISTING PAVEMENT SHALL RECEIVE A TACK COAT OF RS-1 OR RS-2 EMULSIFIED ASPHALT. APPLICATION RATE FOR TACK COAT SHALL BE 0.05 GALLONS PER SQUARE YARD.

APHALTIC PAVEMENT SHALL BE CONSTRUCTED WITH THE LAYERS AND GRADATIONS AS SHOWN IN THE PROPOSED TYPICAL SECTIONS.

CONTRACTOR WILL BE RESPONSIBLE FOR RESHAPING AND SEEDING ANY PREVIOUSLY GRASSED AREAS WHICH ARE DISTURBED BY HIS OPERATION OUTSIDE OF THE NORMAL CONSTRUCTION LIMITS.

THE EROSION CONTROL DEVICES AS SHOWN ON THE PAVING PLAN SHEETS ARE AT SUGGESTED LOCATIONS. THE EXACT LOCATION WILL BE DETERMINED BY THE ENGINEER.

THE CONTRACTOR WILL COORDINATE WITH LOCAL BUSINESSES WHEN ACCESS TO THE BUSINESS WILL BE CLOSED OFF FOR SIDEWALK INSTALLATION AND ANY OTHER WORK THAT MAY IMPACT THE PEDESTRIAN ACCESS.

REMOVAL OF EROSION CONTROL DEVICES SHALL BE INCIDENTAL TO RESPECTIVE BID ITEMS.

STANDARD ABBREVIATIONS

| | | | |
|------|-----------------------|-----|-------------------------|
| BCAM | BLOCK CORNER MONUMENT | MH | MANHOLE |
| IP | IRON PIPE | WV | WATER VALVE |
| RB | REBAR | AV | AUXILIARY VALVE |
| CP | CONTROL POINT | UP | POWER POLE/UTILITY POLE |
| R/W | RIGHT OF WAY | LP | LIGHT POLE |
| PL | PROPERTY LINE | STM | STORM SEWER |
| NTS | NOT TO SCALE | INL | STORM INLET |
| EX | EXISTING | SAN | SANITARY SEWER |
| TBR | TO BE REMOVED | CS | CURB STOP |
| TBA | TO BE ABANDONED | STA | STATION |
| PAVT | PAVEMENT | CL | CENTERLINE |
| CSW | CONCRETE SIDEWALK | T/C | TOP OF CURB |
| DRWY | DRIVEWAY | B/W | BACK OF WALK |
| CONC | CONCRETE | ER | END OF RADIUS |
| ASPH | ASPHALT | PC | POINT OF CURVATURE |
| GRAV | GRAVEL | PI | POINT OF INTERSECTION |
| | | PT | POINT OF TANGENCY |

OTHER CONTACTS

WDNR LIAISON
 BENTON STELZEL
 ENVIRONMENTAL ANALYSIS &
 REVIEW SPECIALIST
 141 NW BARSTOW ST. ROOM 180
 WAUKESHA, WI 53187
 (262) 623-0194
BENTON.STELZEL@WISCONSIN.GOV

WISDOT CONTACT
 MICHAEL BAIRD, PE
 WISDOT SE REGION
 PDS-LOCAL PROGRAMS
 (414) 750-3079
MICHAEL.BAIRD.DOT.WI.GOV

DESIGN CONSULTANT

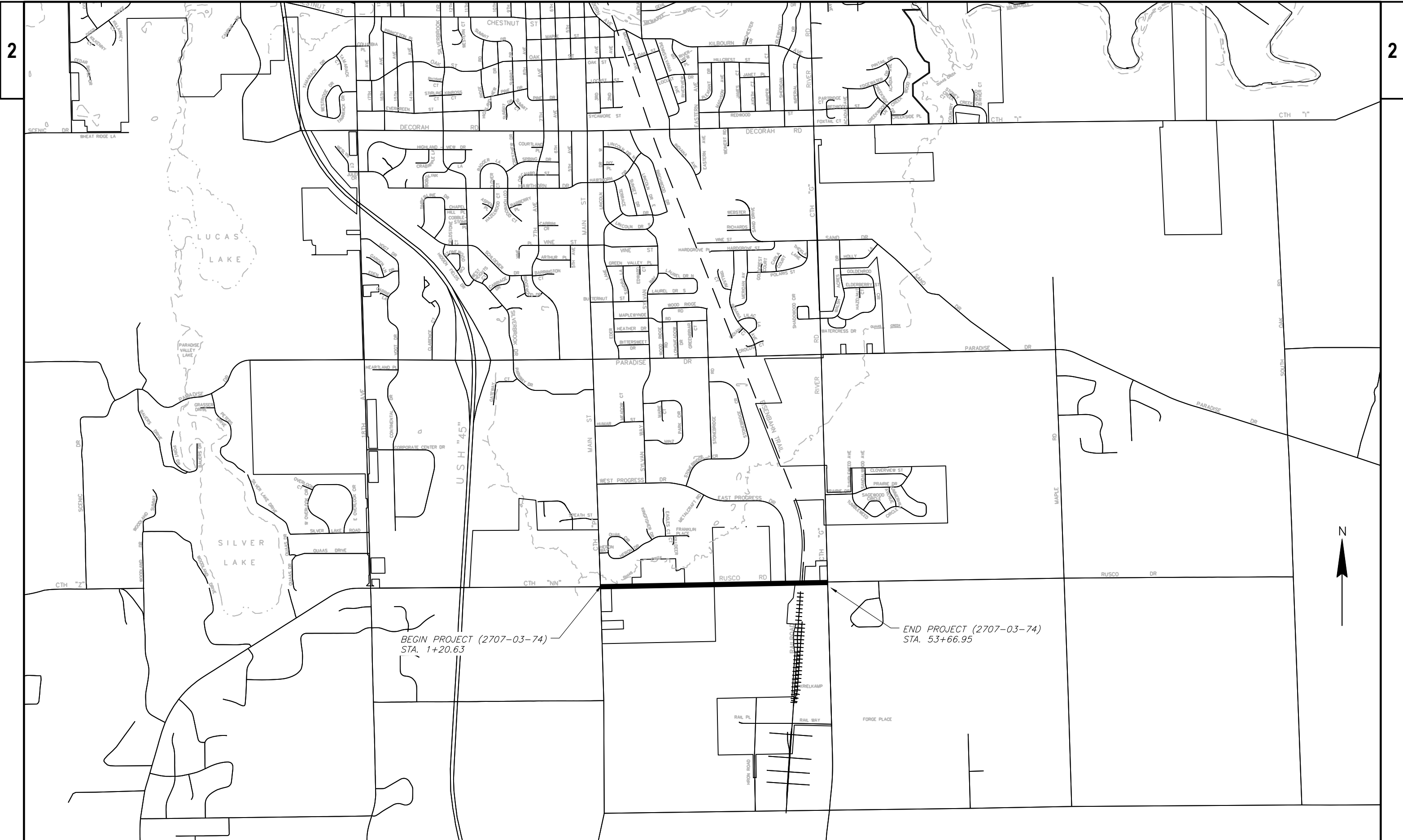
JACKIE KOHN - BORN, PE
 KUNKEL ENGINEERING GROUP
 107 PARALLEL ST.
 BEAVER DAM, WI 53916
 (920) 763-8155
JKOHN@GEO-LOGIC.COM

WASHINGTON CO SURVEYOR

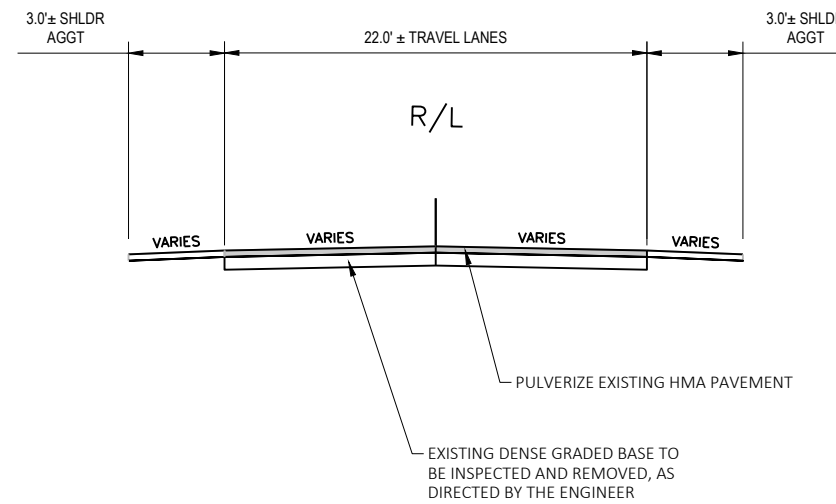
SCOTT SCHMIDT
 900 LANG ST.
 WEST BEND, WI 53090
 (262) 335-6881
SCOTT.SCHMIDT@CO.WASHINGTON.WI.US

ORDER OF SECTION 2 SHEETS

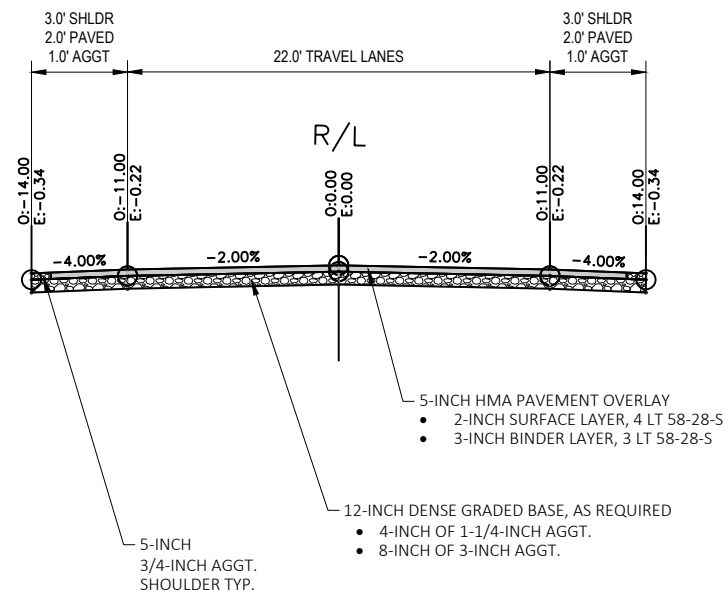
- GENERAL NOTES
- PROJECT OVERVIEW
- TYPICAL SECTIONS
- EROSION AND SEDIMENT CONTROL
- DETOUR PLAN
- ALIGNMENT LAYOUT SURVEY CONTROL



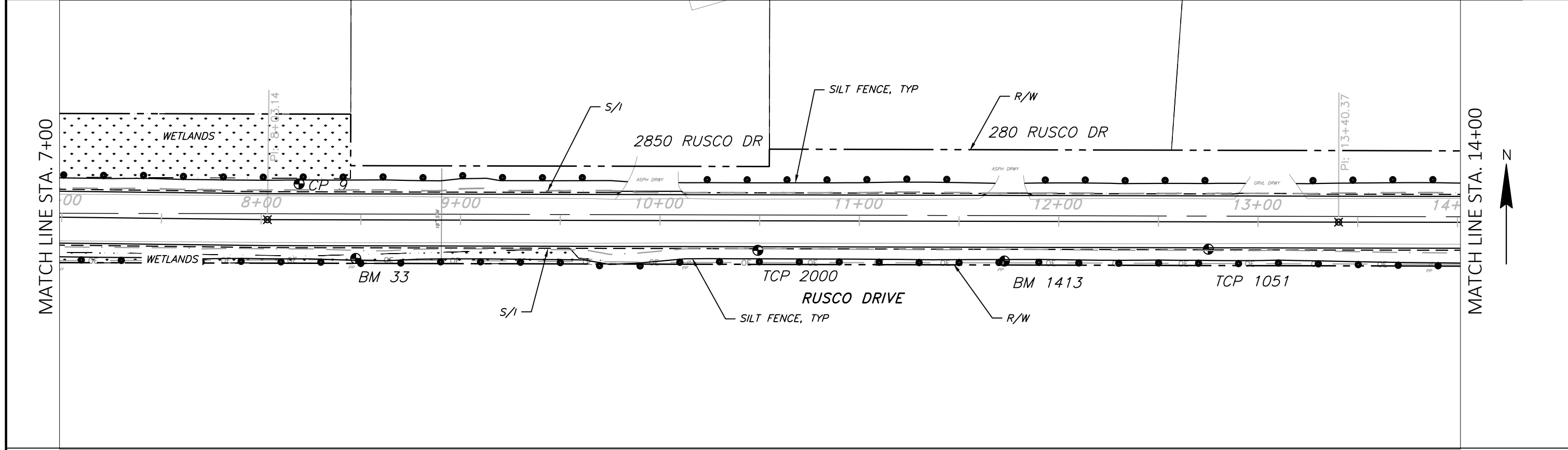
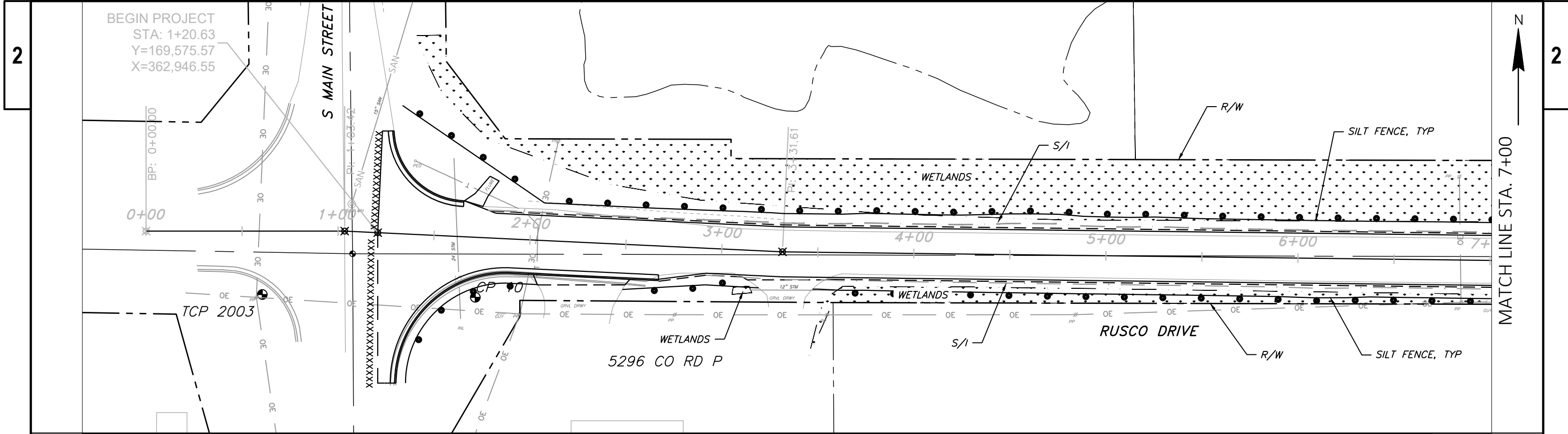
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|------------------------|-------------------------------------|---------------------------|------------------|-----------|----------|
| PROJECT NO: 2707-03-74 | HWY: RUSCO DRIVE, CITY OF WEST BEND | COUNTY: WASHINGTON COUNTY | PROJECT OVERVIEW | SHEET OV1 | E |
|------------------------|-------------------------------------|---------------------------|------------------|-----------|----------|



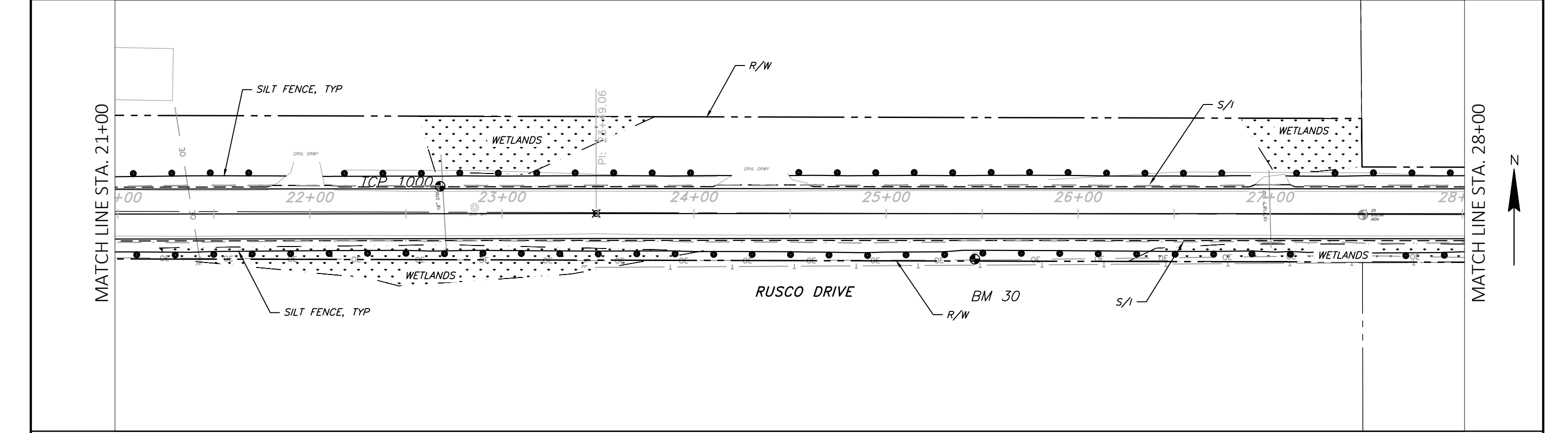
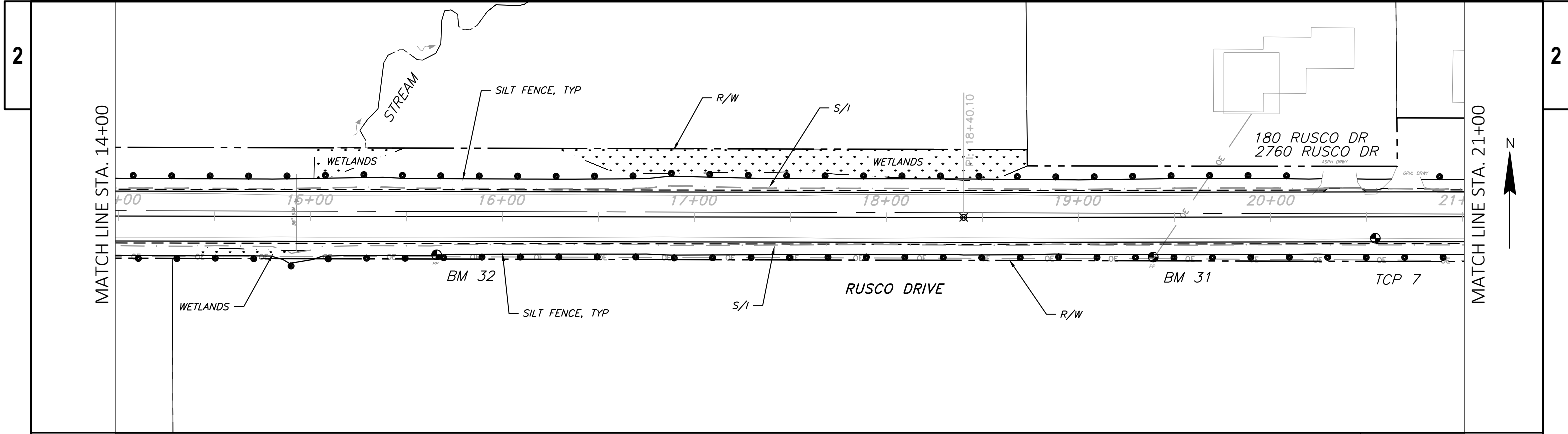
EXISTING TYPICAL SECTION (NTS)
RUSCO DRIVE



FINISHED TYPICAL SECTION (NTS)
RUSCO DRIVE



| | | | | | |
|------------------------|-------------------------------------|---------------------------|-----------------------------------|-----------|---|
| PROJECT NO: 2707-03-74 | HWY: RUSCO DRIVE, CITY OF WEST BEND | COUNTY: WASHINGTON COUNTY | EROSION AND SEDIMENT CONTROL PLAN | SHEET EC1 | E |
|------------------------|-------------------------------------|---------------------------|-----------------------------------|-----------|---|



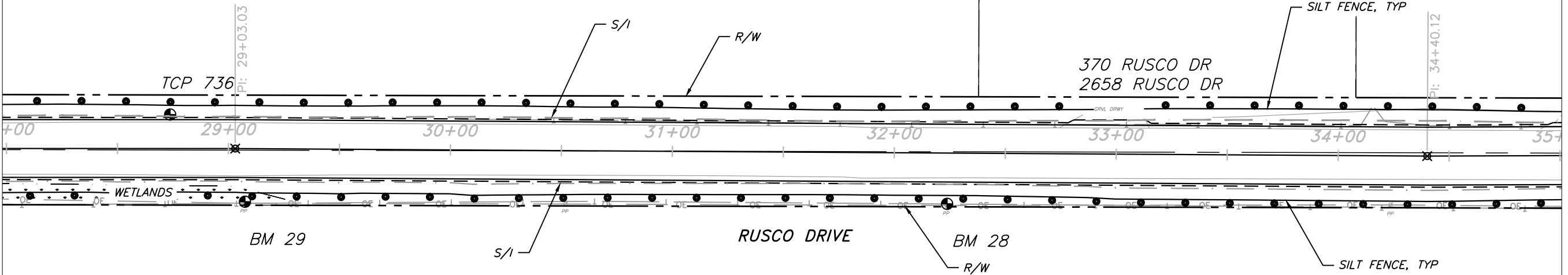
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|------------------------|-------------------------------------|---------------------------|-----------------------------------|-----------|----------|
| PROJECT NO: 2707-03-74 | HWY: RUSCO DRIVE, CITY OF WEST BEND | COUNTY: WASHINGTON COUNTY | EROSION AND SEDIMENT CONTROL PLAN | SHEET EC2 | E |
|------------------------|-------------------------------------|---------------------------|-----------------------------------|-----------|----------|

2

2

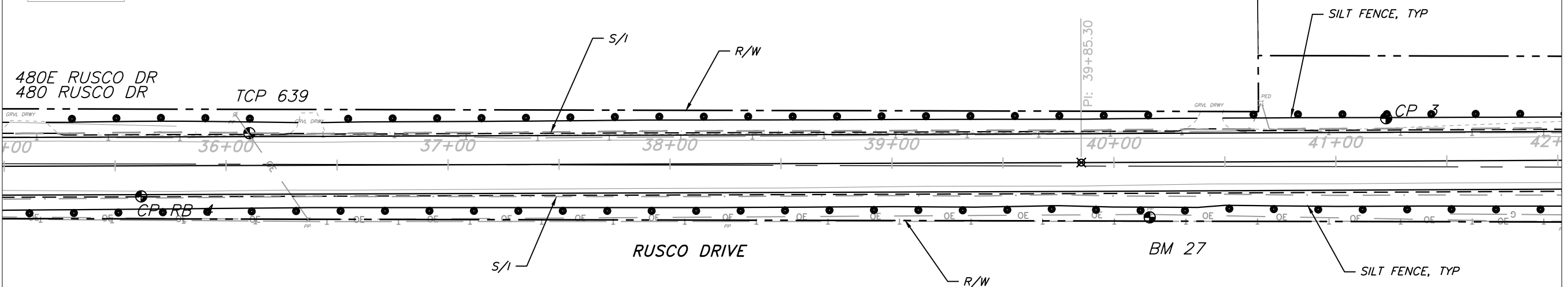
MATCH LINE STA. 28+00

MATCH LINE STA. 35+00

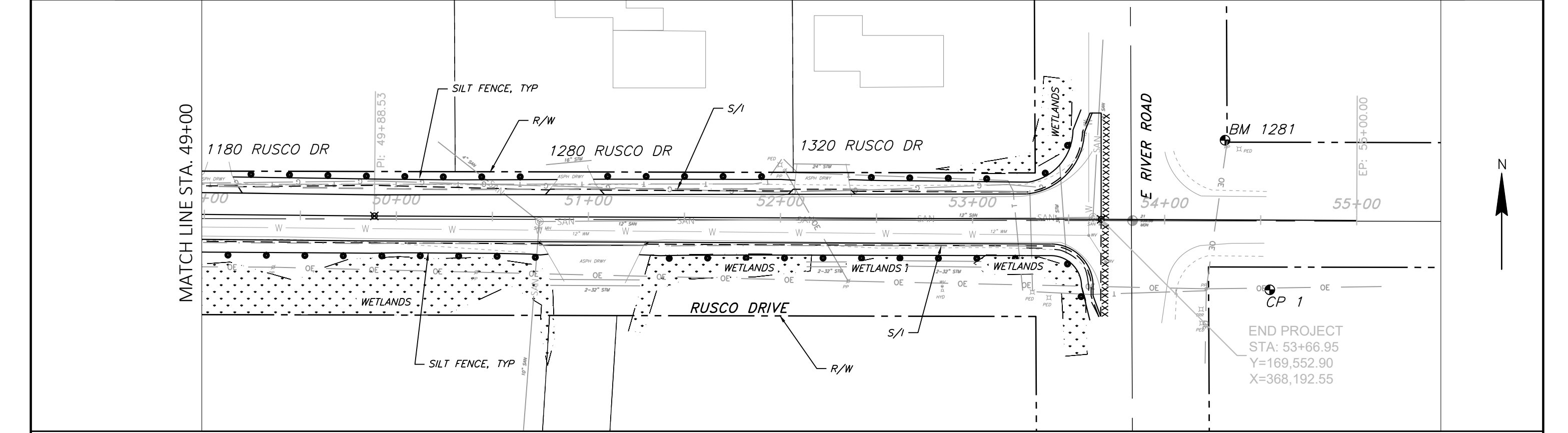
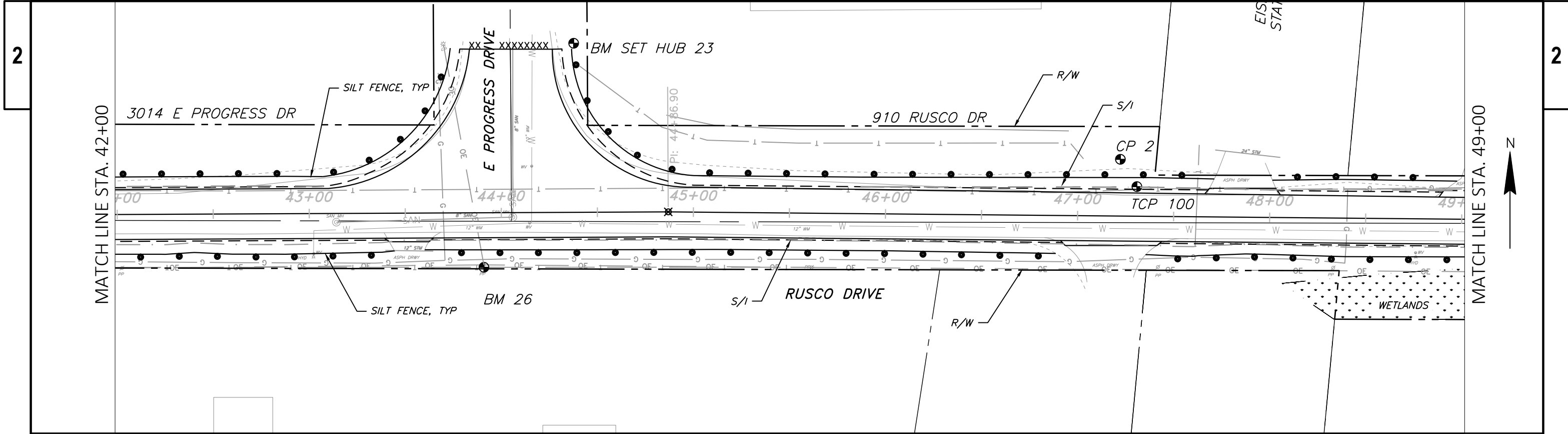


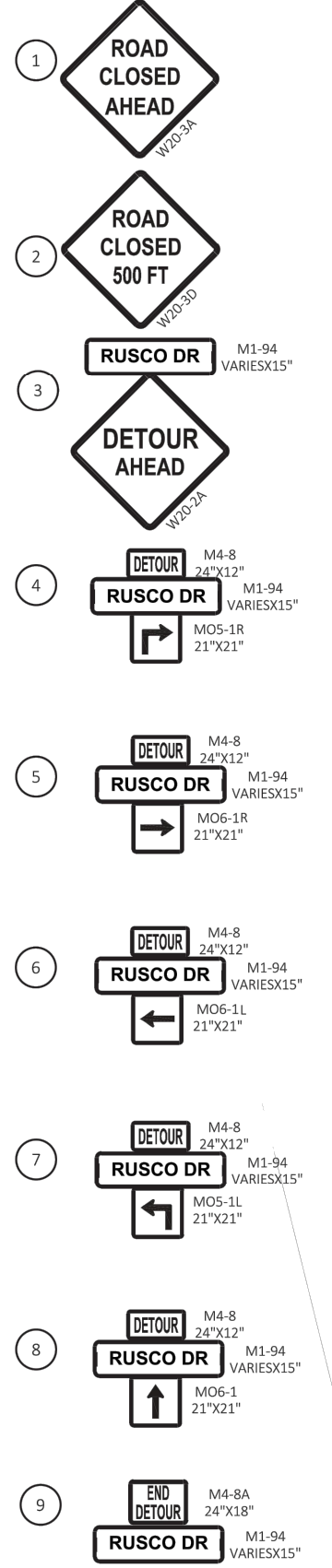
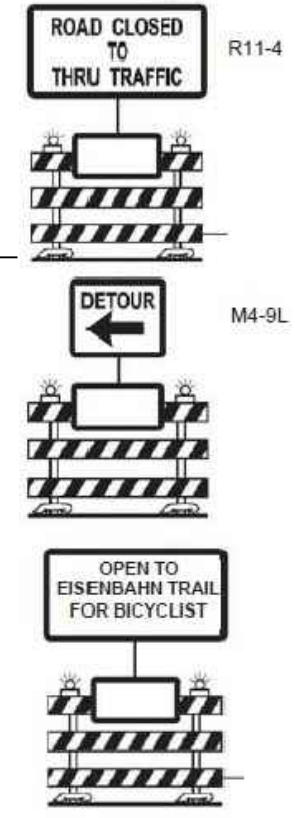
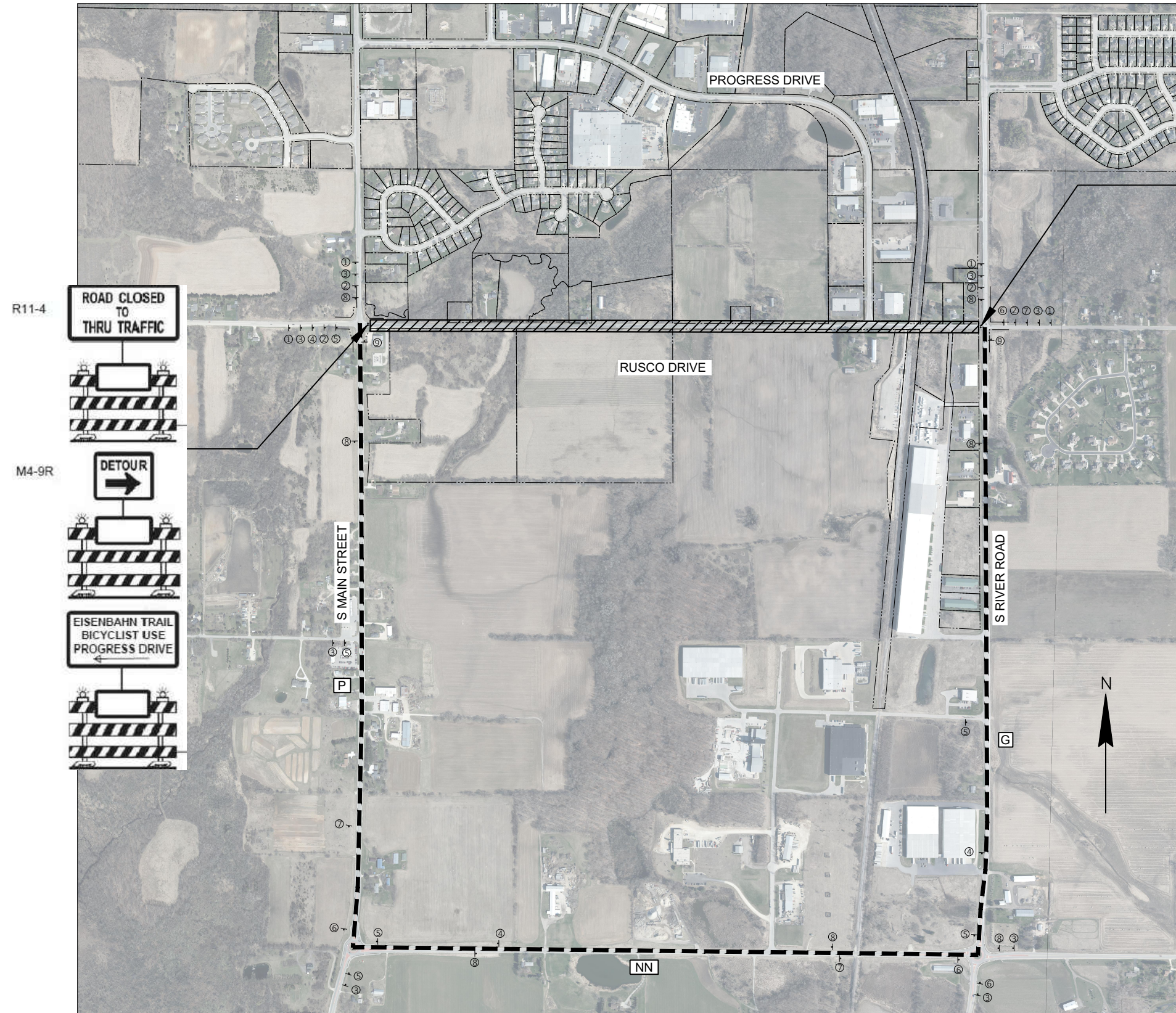
MATCH LINE STA. 35+00

MATCH LINE STA. 42+00



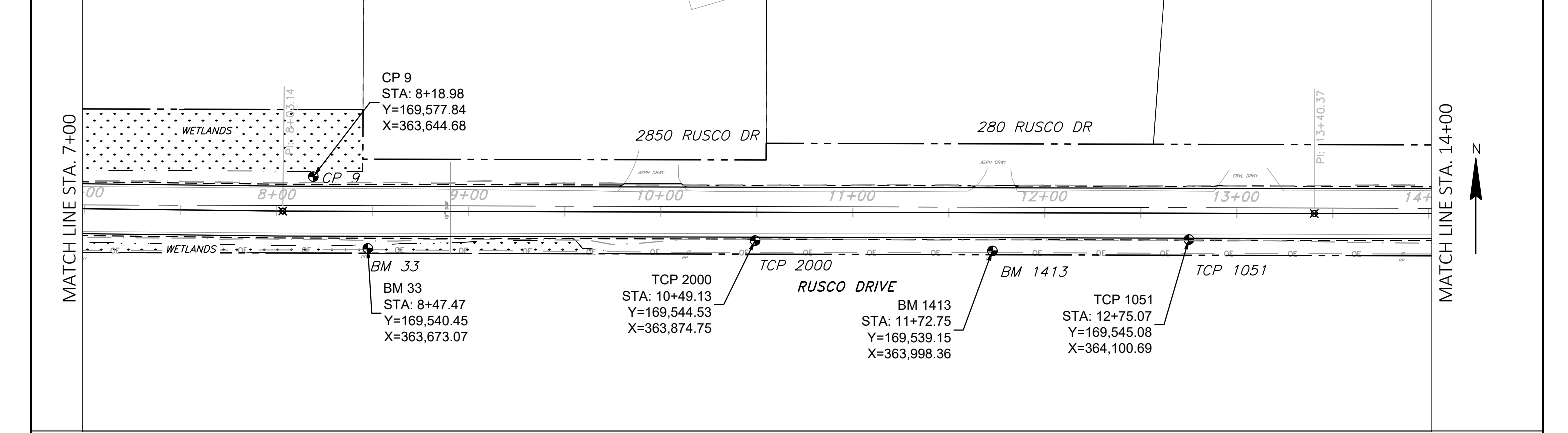
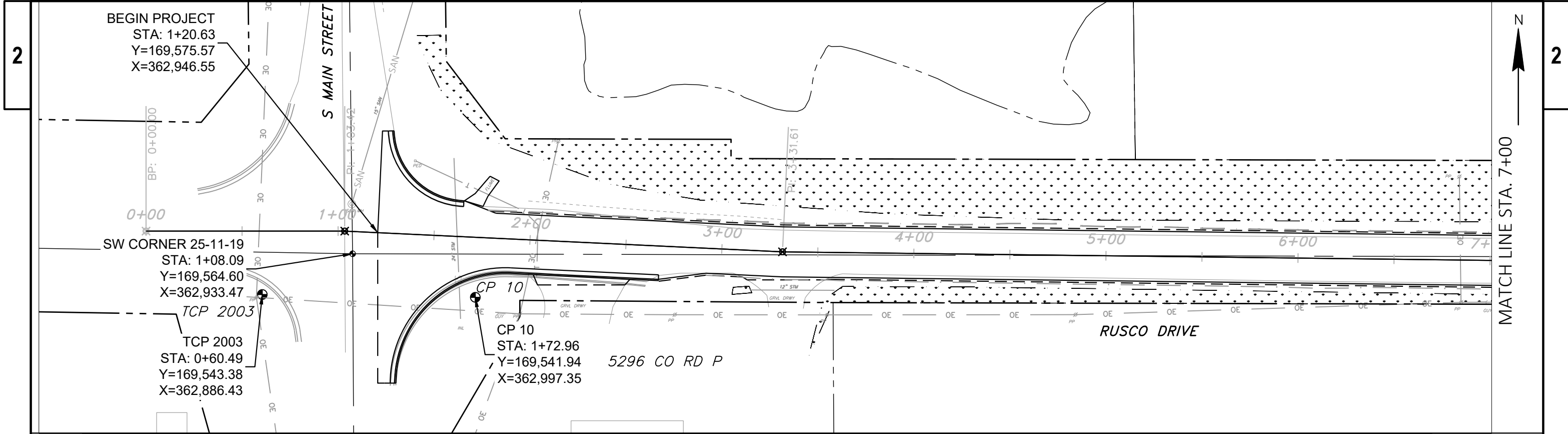
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|------------------------|-------------------------------------|---------------------------|-----------------------------------|-----------|---|
| PROJECT NO: 2707-03-74 | HWY: RUSCO DRIVE, CITY OF WEST BEND | COUNTY: WASHINGTON COUNTY | EROSION AND SEDIMENT CONTROL PLAN | SHEET EC3 | E |
|------------------------|-------------------------------------|---------------------------|-----------------------------------|-----------|---|



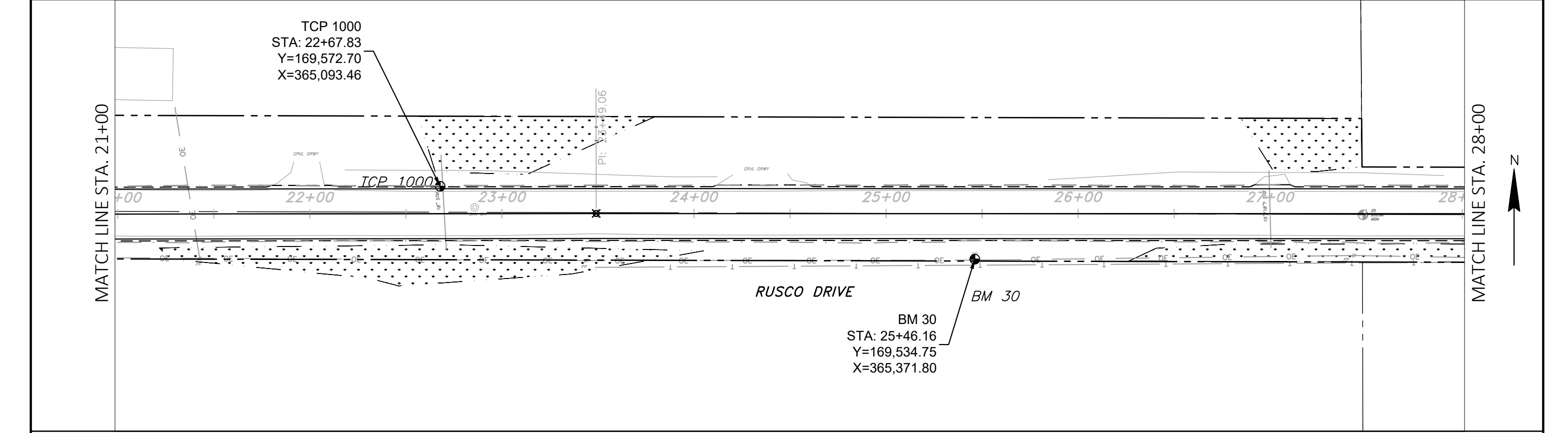
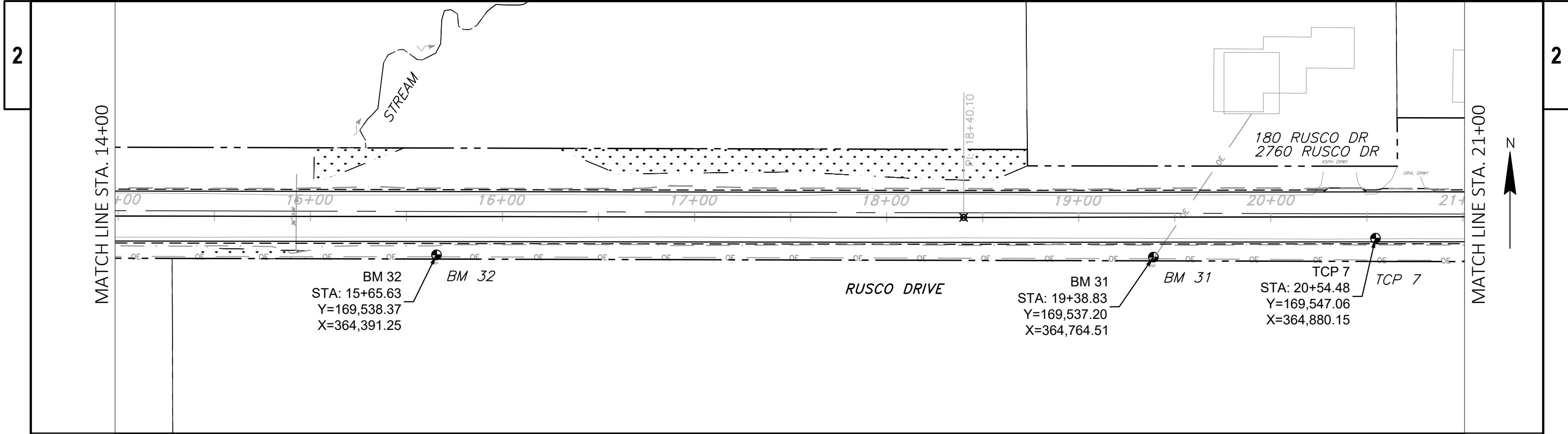


DETOUR MAP NOTES:

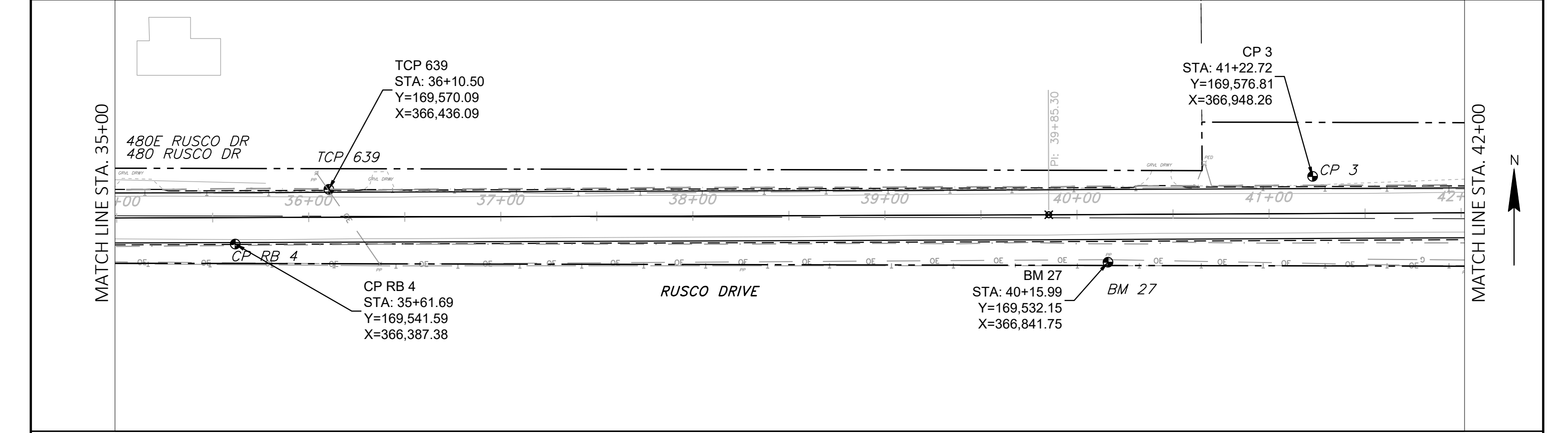
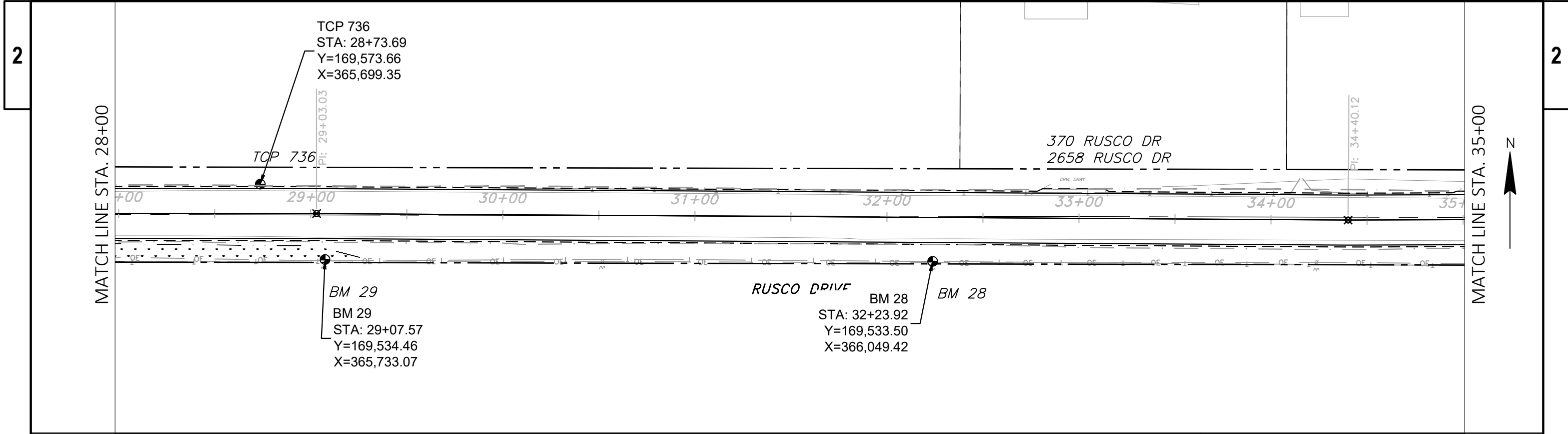
REFER TO WISDOT STANDARD DETAILS SDD 15C02-08a, SDD 15C02-08b AND SDD 15C02-08c FOR BARRICADES AND SIGNS INSTALLATION AND SPACING/ LOCATIONS.
 NO BARRICADES OR SIGNS SHALL BE PLACED WITHIN 50 FEET OF THE RAILROAD RIGHT OF WAY.



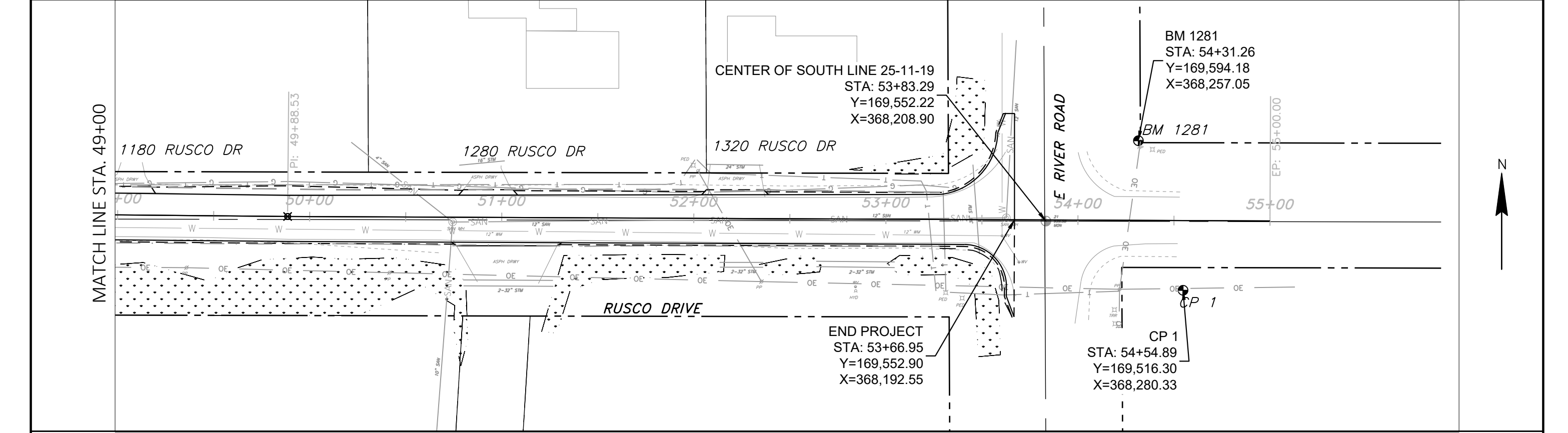
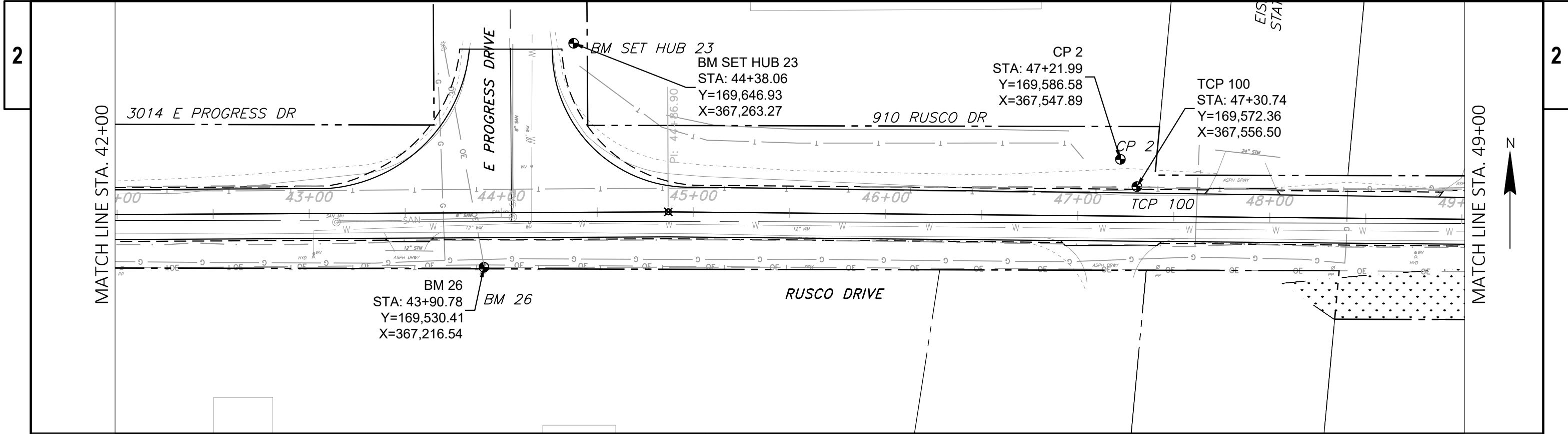
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|------------------------|-------------------------------------|---------------------------|---------------------------------|-----------|---|
| PROJECT NO: 2707-03-74 | HWY: RUSCO DRIVE, CITY OF WEST BEND | COUNTY: WASHINGTON COUNTY | ALIGNMENT LAYOUT SURVEY CONTROL | SHEET AL1 | E |
|------------------------|-------------------------------------|---------------------------|---------------------------------|-----------|---|



| | | | | | |
|------------------------|-------------------------------------|---------------------------|---------------------------------|-----------|---|
| PROJECT NO: 2707-03-74 | HWY: RUSCO DRIVE, CITY OF WEST BEND | COUNTY: WASHINGTON COUNTY | ALIGNMENT LAYOUT SURVEY CONTROL | SHEET AL2 | E |
|------------------------|-------------------------------------|---------------------------|---------------------------------|-----------|---|



| | | | | | |
|------------------------|-------------------------------------|---------------------------|---------------------------------|-----------|---|
| PROJECT NO: 2707-03-74 | HWY: RUSCO DRIVE, CITY OF WEST BEND | COUNTY: WASHINGTON COUNTY | ALIGNMENT LAYOUT SURVEY CONTROL | SHEET AL3 | E |
|------------------------|-------------------------------------|---------------------------|---------------------------------|-----------|---|



| | | | | | |
|------------------------|-------------------------------------|---------------------------|---------------------------------|-----------|---|
| PROJECT NO: 2707-03-74 | HWY: RUSCO DRIVE, CITY OF WEST BEND | COUNTY: WASHINGTON COUNTY | ALIGNMENT LAYOUT SURVEY CONTROL | SHEET AL4 | E |
|------------------------|-------------------------------------|---------------------------|---------------------------------|-----------|---|

Estimate Of Quantities

2707-03-74

| Line | Item | Item Description | Unit | Total | Qty |
|------|----------|--|------|------------|------------|
| 0002 | 204.0115 | Removing Asphaltic Surface Butt Joints | SY | 569.000 | 569.000 |
| 0004 | 204.0150 | Removing Curb & Gutter | LF | 228.000 | 228.000 |
| 0006 | 205.0100 | Excavation Common | CY | 4,004.000 | 4,004.000 |
| 0008 | 213.0100 | Finishing Roadway (project) 01. 2707-03-74 | EACH | 1.000 | 1.000 |
| 0010 | 305.0110 | Base Aggregate Dense 3/4-Inch | TON | 363.000 | 363.000 |
| 0012 | 305.0120 | Base Aggregate Dense 1 1/4-Inch | TON | 8.000 | 8.000 |
| 0014 | 305.0500 | Shaping Shoulders | STA | 93.000 | 93.000 |
| 0016 | 325.0100 | Pulverize and Relay | SY | 16,016.000 | 16,016.000 |
| 0018 | 455.0605 | Tack Coat | GAL | 1,186.000 | 1,186.000 |
| 0020 | 460.2000 | Incentive Density HMA Pavement | DOL | 2,000.000 | 2,000.000 |
| 0022 | 460.5223 | HMA Pavement 3 LT 58-28 S | TON | 3,028.000 | 3,028.000 |
| 0024 | 460.5224 | HMA Pavement 4 LT 58-28 S | TON | 2,019.000 | 2,019.000 |
| 0026 | 465.0120 | Asphaltic Surface Driveways and Field Entrances | TON | 14.000 | 14.000 |
| 0028 | 465.0315 | Asphaltic Flumes | SY | 1.000 | 1.000 |
| 0030 | 601.0411 | Concrete Curb & Gutter 30-Inch Type D | LF | 228.000 | 228.000 |
| 0032 | 611.8110 | Adjusting Manhole Covers | EACH | 2.000 | 2.000 |
| 0034 | 619.1000 | Mobilization | EACH | 1.000 | 1.000 |
| 0036 | 624.0100 | Water | MGAL | 78.000 | 78.000 |
| 0038 | 625.0100 | Topsoil | SY | 3,472.000 | 3,472.000 |
| 0040 | 627.0200 | Mulching | SY | 3,472.000 | 3,472.000 |
| 0042 | 628.1504 | Silt Fence | LF | 10,554.000 | 10,554.000 |
| 0044 | 628.1520 | Silt Fence Maintenance | LF | 10,554.000 | 10,554.000 |
| 0046 | 629.0205 | Fertilizer Type A | CWT | 66.000 | 66.000 |
| 0048 | 630.0140 | Seeding Mixture No. 40 | LB | 66.000 | 66.000 |
| 0050 | 630.0500 | Seed Water | MGAL | 78.000 | 78.000 |
| 0052 | 642.5001 | Field Office Type B | EACH | 1.000 | 1.000 |
| 0054 | 643.5000 | Traffic Control | EACH | 1.000 | 1.000 |
| 0056 | 646.1020 | Marking Line Epoxy 4-Inch | LF | 21,840.000 | 21,840.000 |
| 0058 | 646.6120 | Marking Stop Line Epoxy 18-Inch | LF | 94.000 | 94.000 |
| 0060 | 650.5500 | Construction Staking Curb Gutter and Curb & Gutter | LF | 228.000 | 228.000 |
| 0062 | 650.8000 | Construction Staking Resurfacing Reference | LF | 5,264.000 | 5,264.000 |
| 0064 | 650.9911 | Construction Staking Supplemental Control (project) 01. 2707-03-74 | EACH | 1.000 | 1.000 |
| 0066 | ASP.1T0A | On-the-Job Training Apprentice at \$5.00/HR | HRS | 300.000 | 300.000 |
| 0068 | ASP.1T0G | On-the-Job Training Graduate at \$5.00/HR | HRS | 300.000 | 300.000 |

3

3

REMOVE ASPHALTIC SURFACE BUTT JOINTS

| STATION | - STATION | LOCATION | SY |
|---------------|-----------|------------|-----|
| 1+20 | | LEFT-RIGHT | 149 |
| 10+00 | | LEFT | 29 |
| 11+75 | | LEFT | 22 |
| 20+37 | | LEFT | 26 |
| 43+50 | | RIGHT | 23 |
| 44+06 | | LEFT | 48 |
| 47+25 | | RIGHT | 53 |
| 47+90 | | LEFT | 39 |
| 49+00 | | LEFT | 30 |
| 51+00 | | LEFT | 29 |
| 51+00 | | RIGHT | 58 |
| 52+25 | | LEFT | 33 |
| 53+66 | | LEFT-RIGHT | 30 |
| TOTALS | | | 569 |

REMOVE CURB AND GUTTER

| STATION | - STATION | LOCATION | LF |
|---------------|-----------|----------|-----|
| 1+26 | - 1+64 | LEFT | 58 |
| 1+33 | - 2+68 | RIGHT | 170 |
| TOTALS | | | 228 |

EXCAVATION COMMON

| STATION | - STATION | LOCATION | CY |
|---------------|-----------|----------|------|
| 1+03 | - 53+67 | MAINLINE | 4004 |
| TOTALS | | | 4004 |

FINISHING ROADWAY (PROJECT)

| LOCATION | EA | |
|---------------|----|---|
| RUSCO DRIVE | 1 | |
| TOTALS | | 1 |

BASE AGGREGATE DENSE 3/4-INCH

| STATION | - STATION | LOCATION | TON |
|---------------|-----------|----------|-----|
| 1+77 | - 43+79 | LEFT | 140 |
| 2+68 | - 53+66 | RIGHT | 179 |
| 2+28 | | RIGHT | 2 |
| 13+00 | | LEFT | 1 |
| 20+75 | | LEFT | 1 |
| 22+00 | | LEFT | 1 |
| 24+30 | | LEFT | 2 |
| 27+00 | | LEFT | 1 |
| 33+00 | | LEFT | 1 |
| 35+00 | | LEFT | 1 |
| 36+30 | | LEFT | 1 |
| 40+50 | | LEFT | 1 |
| 44+32 | - 53+61 | LEFT | 30 |
| 47+25 | | RIGHT | 2 |
| TOTALS | | | 363 |

BASE AGGREGATE DENSE 1 1/4-INCH

| STATION | - STATION | LOCATION | TON |
|---------------|-----------|----------|-----|
| 1+26 | - 1+64 | LEFT | 2 |
| 1+33 | - 2+68 | RIGHT | 6 |
| TOTALS | | | 8 |

SHAPING SHOULDERS

| STATION | - STATION | LOCATION | STA |
|---------------|-----------|----------|------|
| 1+77 | - 43+79 | LEFT | 42.1 |
| 2+68 | - 53+66 | RIGHT | 51.0 |
| TOTALS | | | 93 |

PULVERIZE AND RELAY

| STATION | - STATION | LOCATION | SY |
|---------------|-----------|----------|-------|
| 1+03 | - 53+67 | MAINLINE | 16016 |
| TOTALS | | | 16016 |

TACK COAT

| STATION | - STATION | LOCATION | GAL |
|---------------|-----------|----------|------|
| 1+03 | - 53+67 | MAINLINE | 1177 |
| 0+50 | | LEFT | 3 |
| 1+17 | | LEFT | 2 |
| 3+32 | | LEFT | 0 |
| 3+80 | | LEFT | 2 |
| 4+30 | | LEFT | 2 |
| 4+79 | | LEFT | 0 |
| TOTALS | | | 1186 |

HMA PAVEMENT 3 LT 58-28 S

| STATION | - STATION | LOCATION | TON |
|---------------|-----------|----------|------|
| 1+03 | - 53+67 | MAINLINE | 3028 |
| TOTALS | | | 3028 |

HMA PAVEMENT 4 LT 58-28 S

| STATION | - STATION | LOCATION | TON |
|---------------|-----------|----------|------|
| 1+03 | - 53+67 | MAINLINE | 2019 |
| TOTALS | | | 2019 |

ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES

| STATION | - STATION | LOCATION | TON |
|---------------|-----------|----------|-----|
| 10+00 | | LEFT | 1 |
| 11+75 | | LEFT | 1 |
| 20+37 | | LEFT | 1 |
| 43+50 | | RIGHT | 1 |
| 44+06 | | LEFT | 2 |
| 47+25 | | RIGHT | 2 |
| 47+90 | | LEFT | 1 |
| 49+00 | | LEFT | 1 |
| 51+00 | | LEFT | 1 |
| 51+00 | | RIGHT | 2 |
| 52+25 | | LEFT | 1 |
| TOTALS | | | 14 |

ASPHALTIC FLUMES

| STATION | - STATION | LOCATION | SY |
|---------------|-----------|----------|----|
| 1+64 | | LEFT | 1 |
| TOTALS | | | 1 |

CONCRETE CURB AND GUTTER, 30 INCH, TYPE D

| STATION | - STATION | LOCATION | LF |
|---------------|-----------|----------|-----|
| 1+26 | - 1+64 | LEFT | 58 |
| 1+33 | - 2+68 | RIGHT | 170 |
| TOTALS | | | 228 |

ADJUST MANHOLE COVERS

| STATION | - STATION | LOCATION | EA |
|---------------|-----------|----------|----|
| 50+90 | | RIGHT | 1 |
| 53+65 | | RIGHT | 1 |
| TOTALS | | | 2 |

MOBILIZATION

| LOCATION | EA | |
|---------------|----|---|
| RUSCO DRIVE | 1 | |
| TOTALS | | 1 |

WATER

| STATION | - STATION | LOCATION | MGAL |
|---------------|-----------|---------------|------|
| 1+03 | - 53+67 | UNDISTRIBUTED | 78 |
| TOTALS | | | 78 |

3

3

TOPSOIL

| STATION | - STATION | LOCATION | 625.0100 SY |
|---------------|-----------|----------|----------------|
| 1+26 | - 1+64 | LEFT | 21 |
| 1+33 | - 2+68 | RIGHT | 60 |
| 1+77 | - 43+79 | LEFT | 1351 |
| 2+68 | - 53+66 | RIGHT | 1733 |
| 44+32 | - 53+61 | LEFT | 307 |
| TOTALS | | | 3472 |

MULCHING

| STATION | - STATION | LOCATION | 627.0200 SY |
|---------------|-----------|----------|----------------|
| 1+26 | - 1+64 | LEFT | 21 |
| 1+33 | - 2+68 | RIGHT | 60 |
| 1+77 | - 43+79 | LEFT | 1351 |
| 2+68 | - 53+66 | RIGHT | 1733 |
| 44+32 | - 53+61 | LEFT | 307 |
| TOTALS | | | 3472 |

SILT FENCE

| STATION | - STATION | LOCATION | 628.1504 LF |
|---------------|-----------|----------|----------------|
| 1+03 | - 53+67 | LEFT | 5200 |
| 1+03 | - 53+67 | RIGHT | 5354 |
| TOTALS | | | 10554 |

SILT FENCE MAINEANCE

| STATION | - STATION | LOCATION | 628.152 LF |
|---------------|-----------|----------|---------------|
| 1+03 | - 53+67 | LEFT | 5200 |
| 1+03 | - 53+67 | RIGHT | 5354 |
| TOTALS | | | 10554 |

FERTILIZER TYPE A

| STATION | - STATION | LOCATION | 629.0205 CWT |
|---------------|-----------|---------------|-----------------|
| 1+03 | - 53+67 | UNDISTRIBUTED | 66 |
| TOTALS | | | 66 |

SEEDING MIXTURE NO. 40

| STATION | - STATION | LOCATION | 630.0140 LB |
|---------------|-----------|----------|----------------|
| 1+26 | - 1+64 | LEFT | 1 |
| 1+33 | - 2+68 | RIGHT | 2 |
| 1+77 | - 43+79 | LEFT | 25 |
| 2+68 | - 53+66 | RIGHT | 32 |
| 44+32 | - 53+61 | LEFT | 6 |
| TOTALS | | | 66 |

SEED WATER

| STATION | - STATION | LOCATION | 630.0500 MGAL |
|---------------|-----------|---------------|------------------|
| 1+03 | - 53+67 | UNDISTRIBUTED | 78 |
| TOTALS | | | 78 |

FIELD OFFICE TYPE B

| LOCATION | 642.5001 EA | |
|---------------|----------------|---|
| RUSCO DRIVE | 1 | |
| TOTALS | | 1 |

TRAFFIC CONTROL

| LOCATION | 643.5000 EA | |
|---------------|----------------|---|
| RUSCO DRIVE | 1 | |
| TOTALS | | 1 |

MARKING LINE EPOXY 4-INCH

| STATION | - STATION | LOCATION | 646.0120 LF |
|---------------|-----------|--------------|----------------|
| 1+64 | - 53+67 | CENTERLINE | 10920 |
| 1+64 | - 53+67 | EDGE LINE LT | 5460 |
| 1+64 | - 53+67 | EDGE LINE RT | 5460 |
| TOTALS | | | 21840 |

MARKING STOP LINE EPOXY 18-INCH

| STATION | - STATION | LOCATION | 646.6120 LF |
|---------------|-----------|----------|----------------|
| 1+64 | | LEFT | 26 |
| 44+00 | | LEFT | 42 |
| 53+50 | | RIGHT | 26 |
| TOTALS | | | 94 |

CONSTRUCTION STAKING CURB GUTTER AND CURB & GUTTER

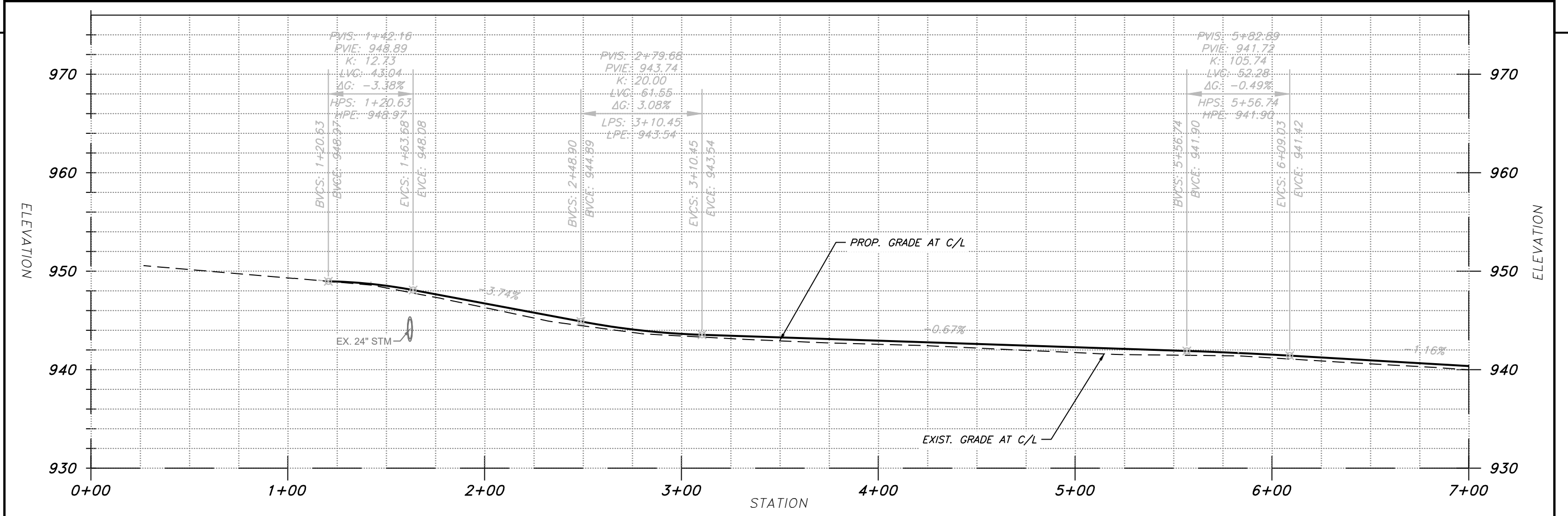
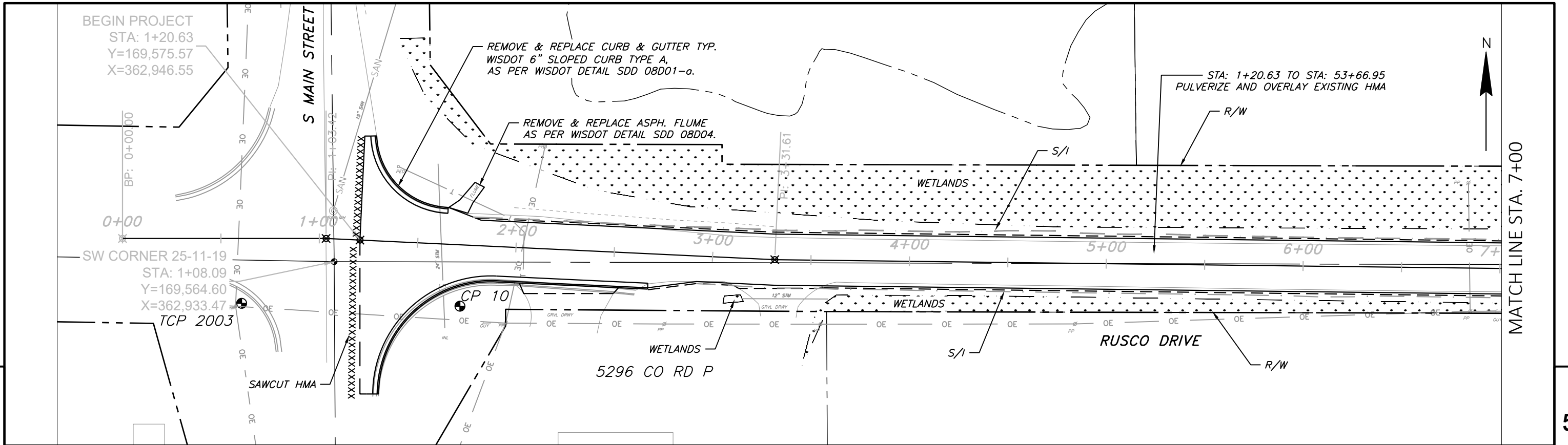
| STATION | - STATION | LOCATION | 650.5500 LF |
|---------------|-----------|----------|----------------|
| 1+26 | - 1+64 | LEFT | 58 |
| 1+33 | - 2+68 | RIGHT | 170 |
| TOTALS | | | 228 |

CONSTRUCTION STAKING RESURFACING REFERENCE

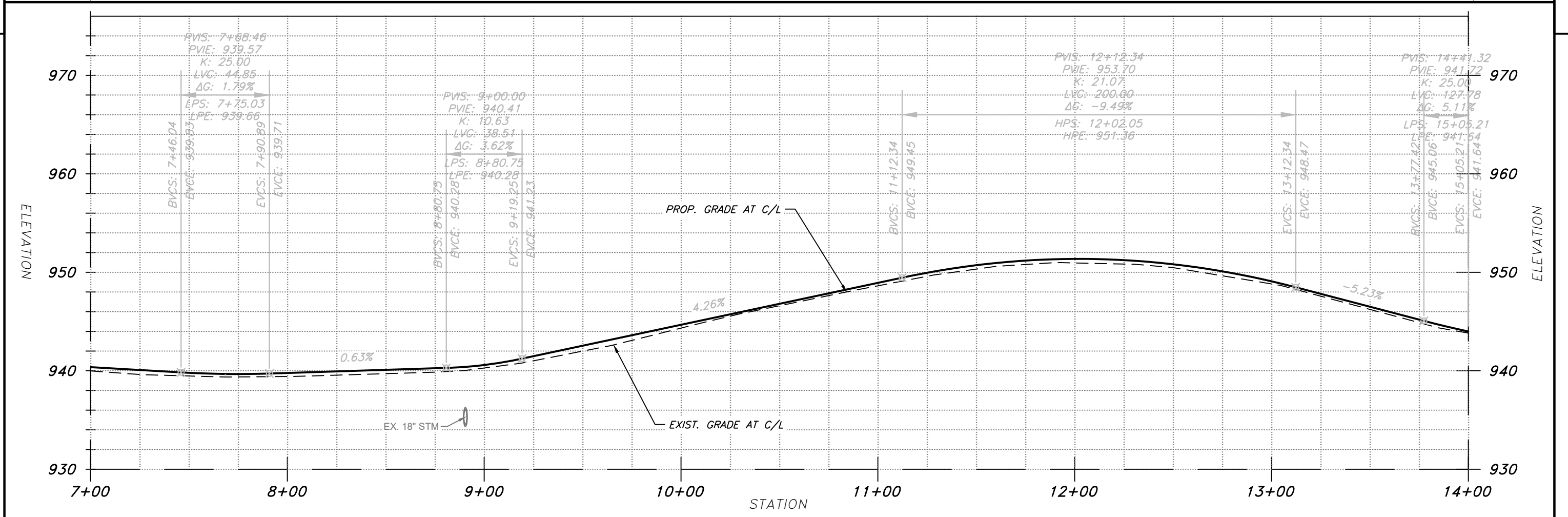
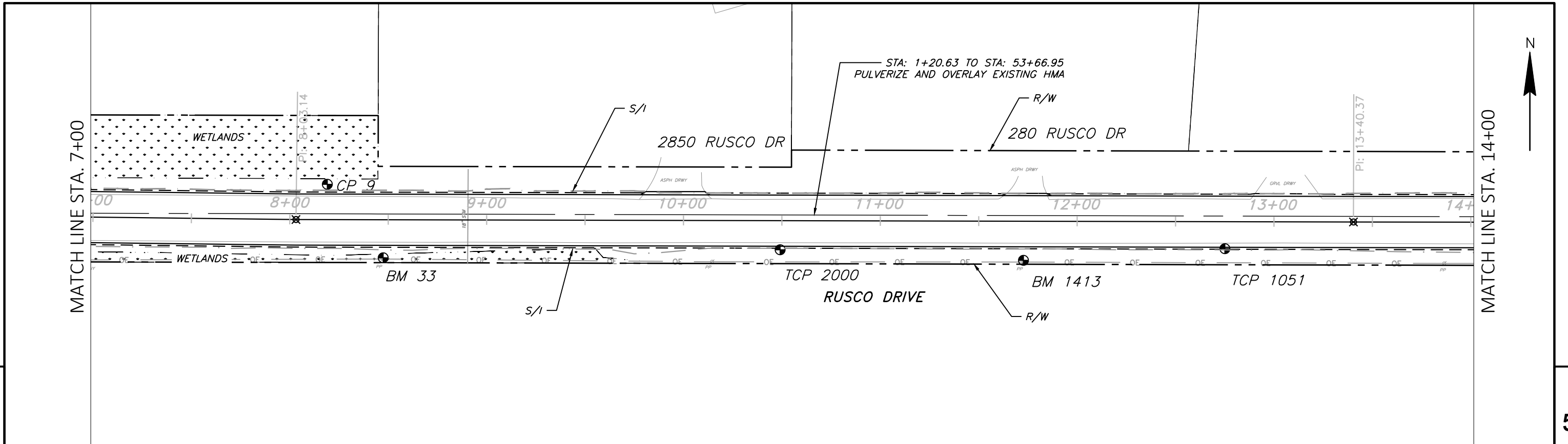
| STATION | - STATION | LOCATION | 650.8000 LF |
|---------------|-----------|----------|----------------|
| 1+03 | - 53+67 | MAINLINE | 5264 |
| TOTALS | | | 5264 |

CONSTRUCTION STAKING SUPPLEMENTAL CONTROL (PROJECT)

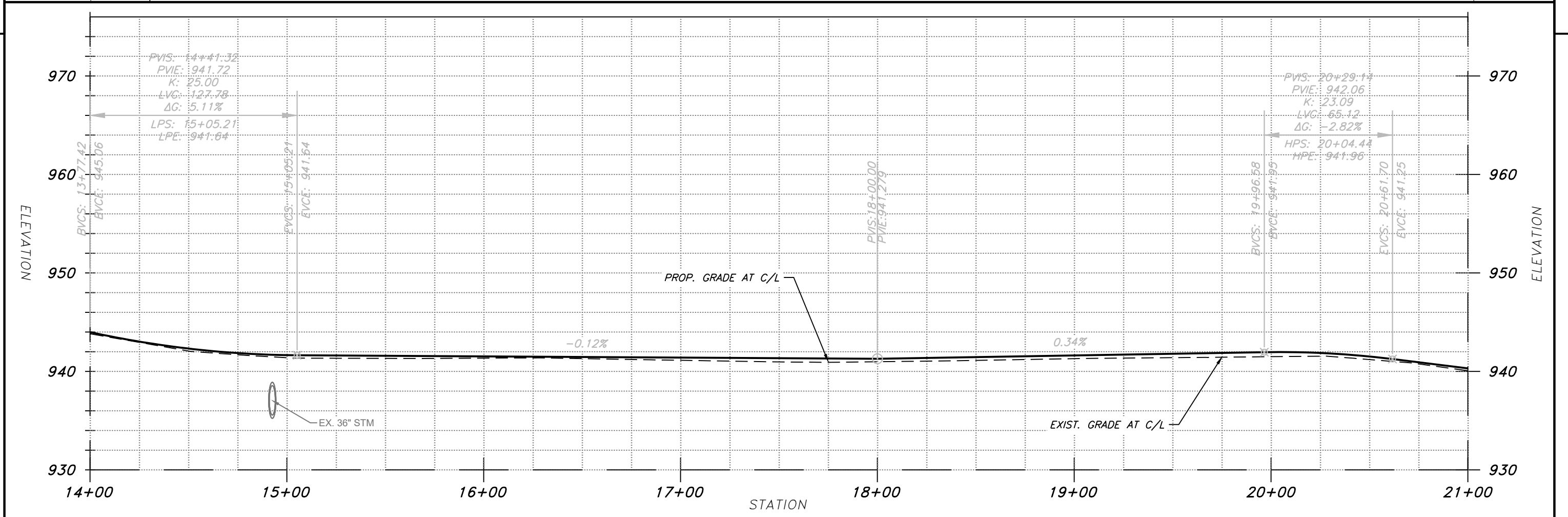
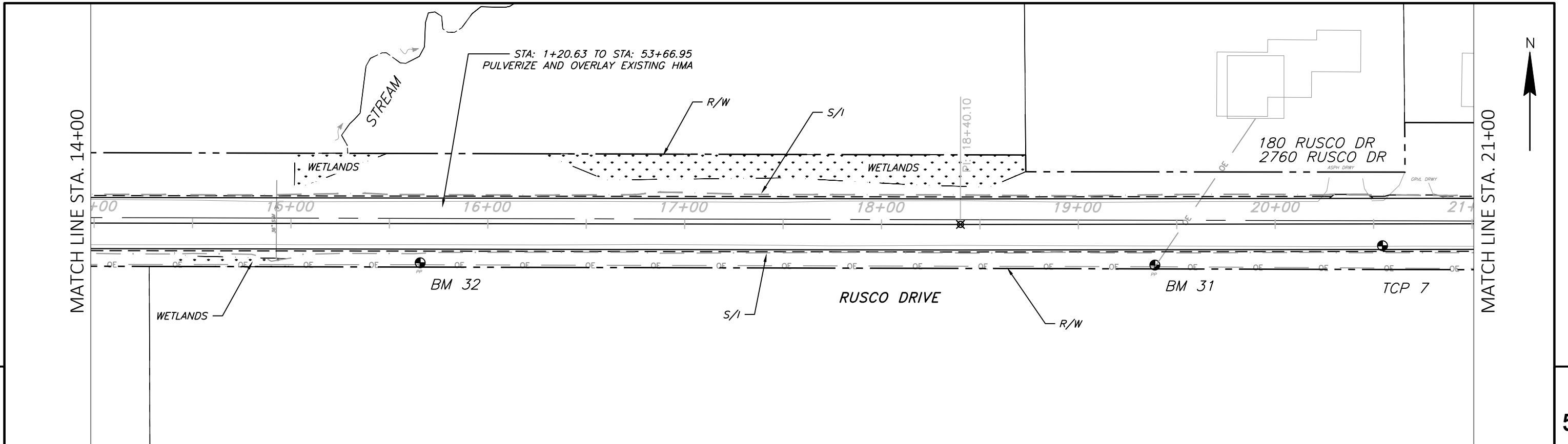
| LOCATION | 650.991 EA | |
|---------------|---------------|---|
| RUSCO DRIVE | 1 | |
| TOTALS | | 1 |



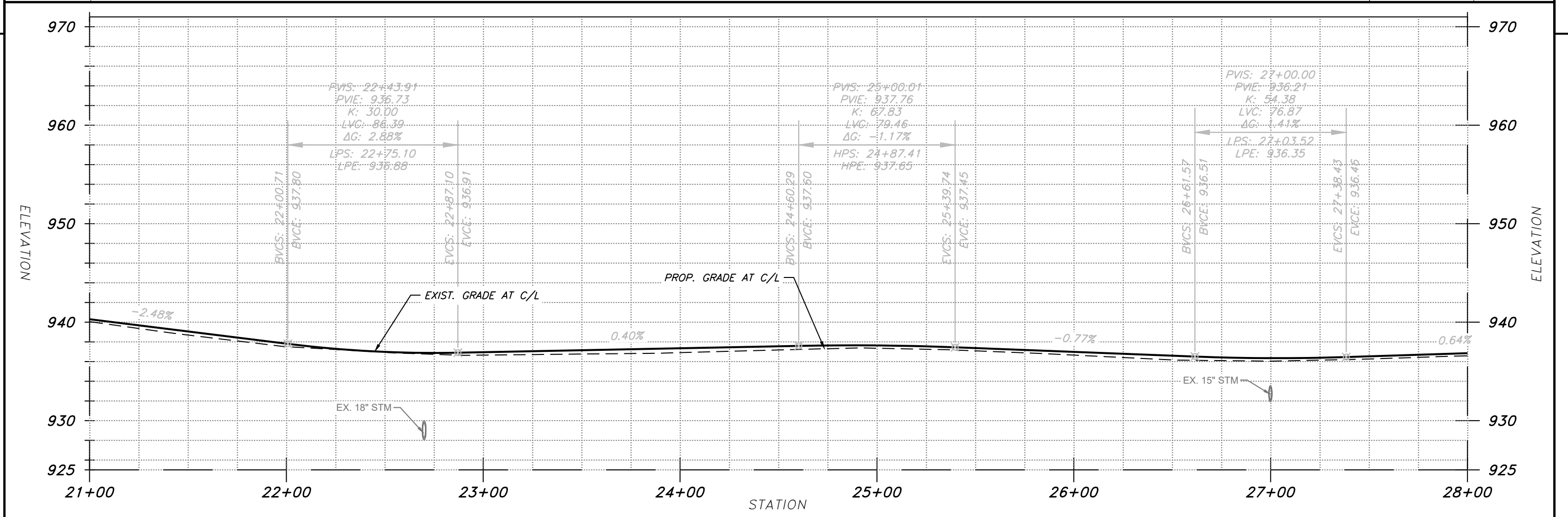
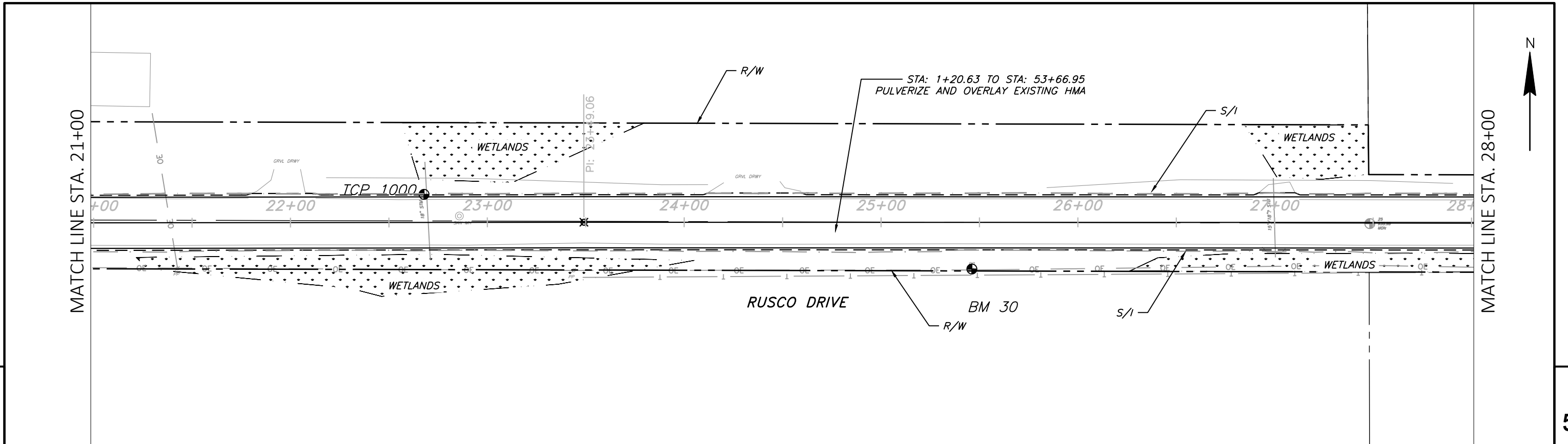
PROJECT NO: 2707-03-74 HWY: RUSCO DRIVE, CITY OF WEST BEND COUNTY: WASHINGTON COUNTY PLAN AND PROFILE: RUSCO DRIVE SHEET PP1 E



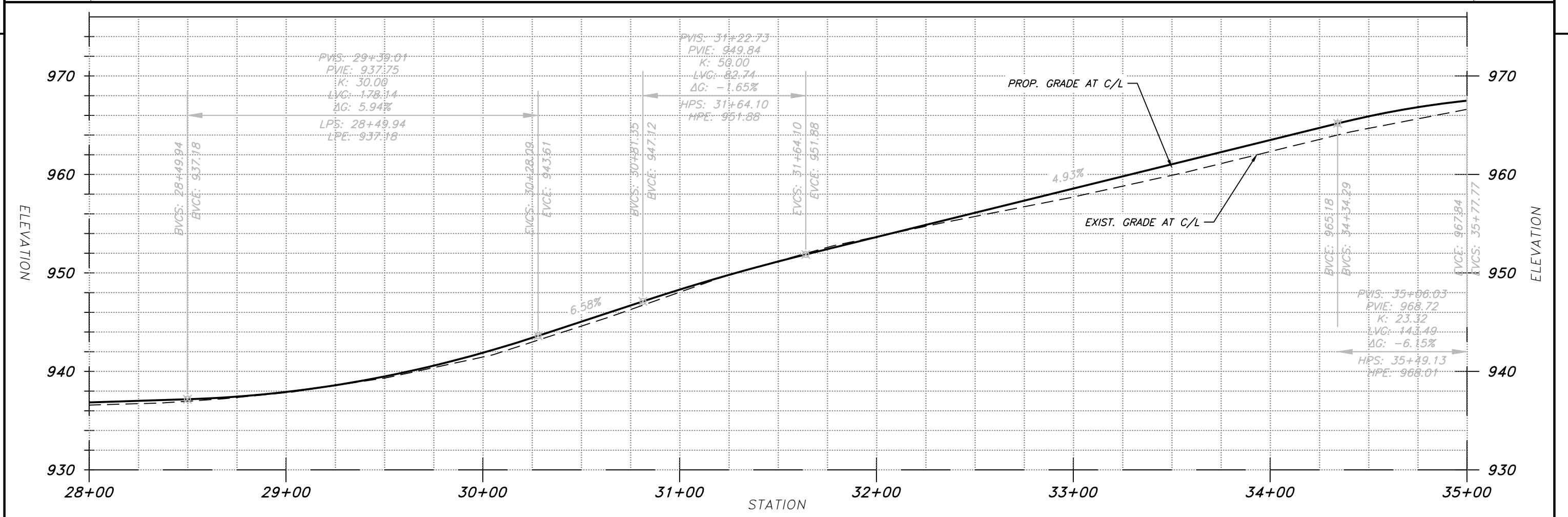
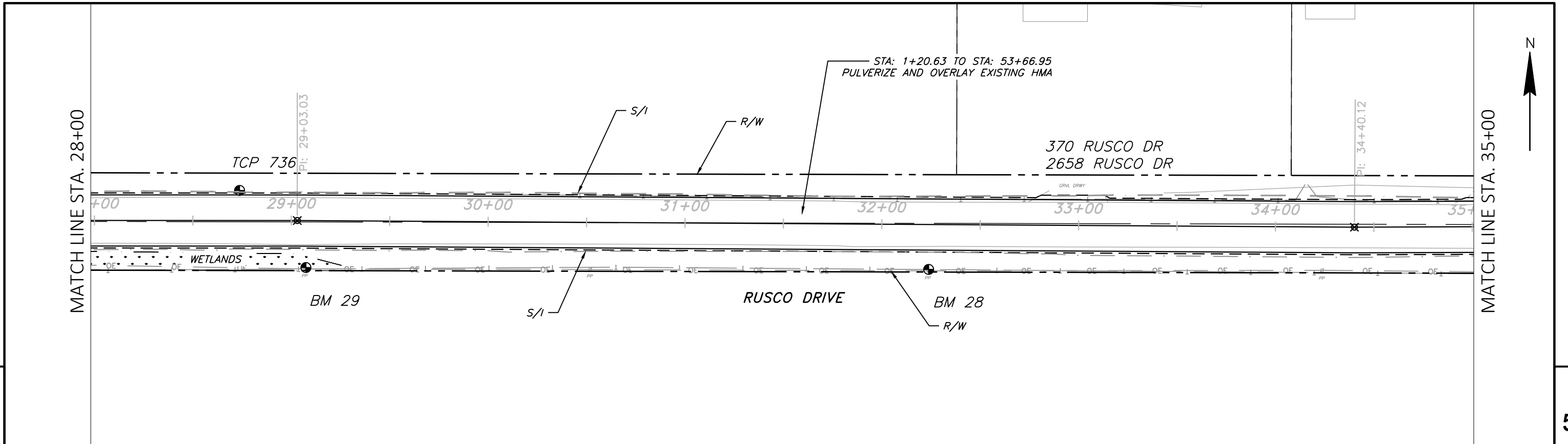
| | | | | | |
|------------------------|-------------------------------------|---------------------------|-------------------------------|-----------|---|
| PROJECT NO: 2707-03-74 | HWY: RUSCO DRIVE, CITY OF WEST BEND | COUNTY: WASHINGTON COUNTY | PLAN AND PROFILE: RUSCO DRIVE | SHEET PP2 | E |
|------------------------|-------------------------------------|---------------------------|-------------------------------|-----------|---|



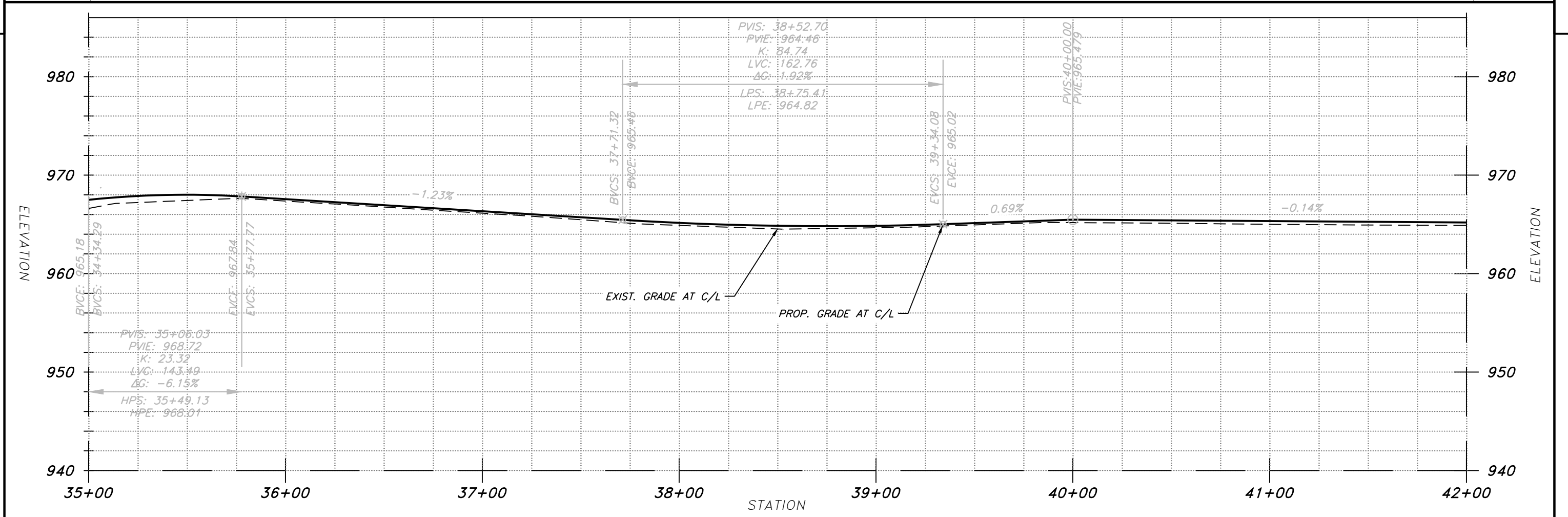
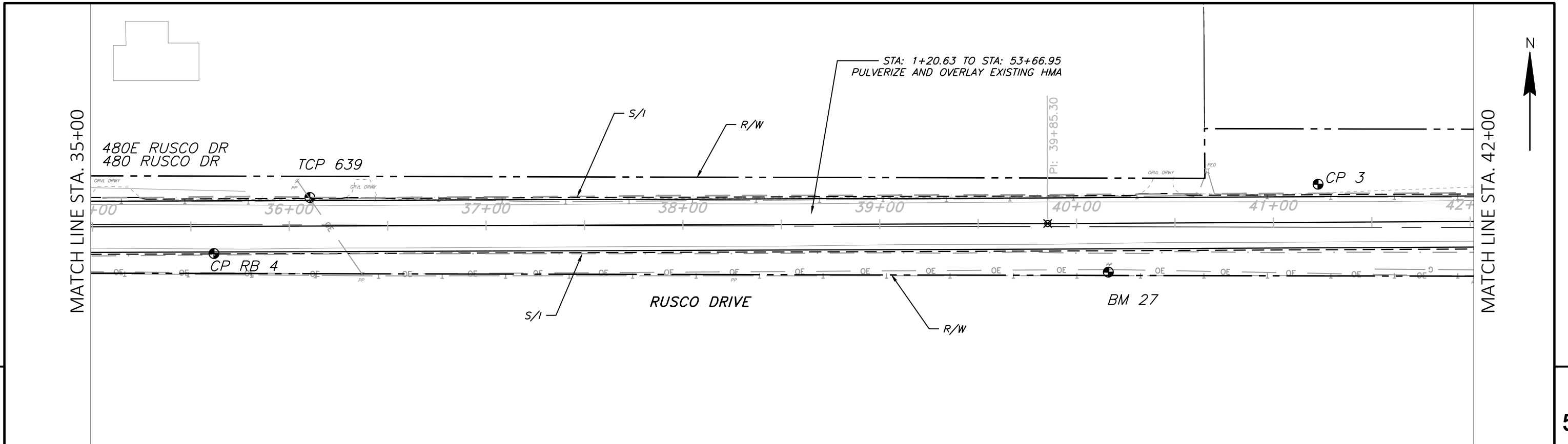
| | | | | | |
|------------------------|-------------------------------------|---------------------------|-------------------------------|-----------|---|
| PROJECT NO: 2707-03-74 | HWY: RUSCO DRIVE, CITY OF WEST BEND | COUNTY: WASHINGTON COUNTY | PLAN AND PROFILE: RUSCO DRIVE | SHEET PP3 | E |
|------------------------|-------------------------------------|---------------------------|-------------------------------|-----------|---|



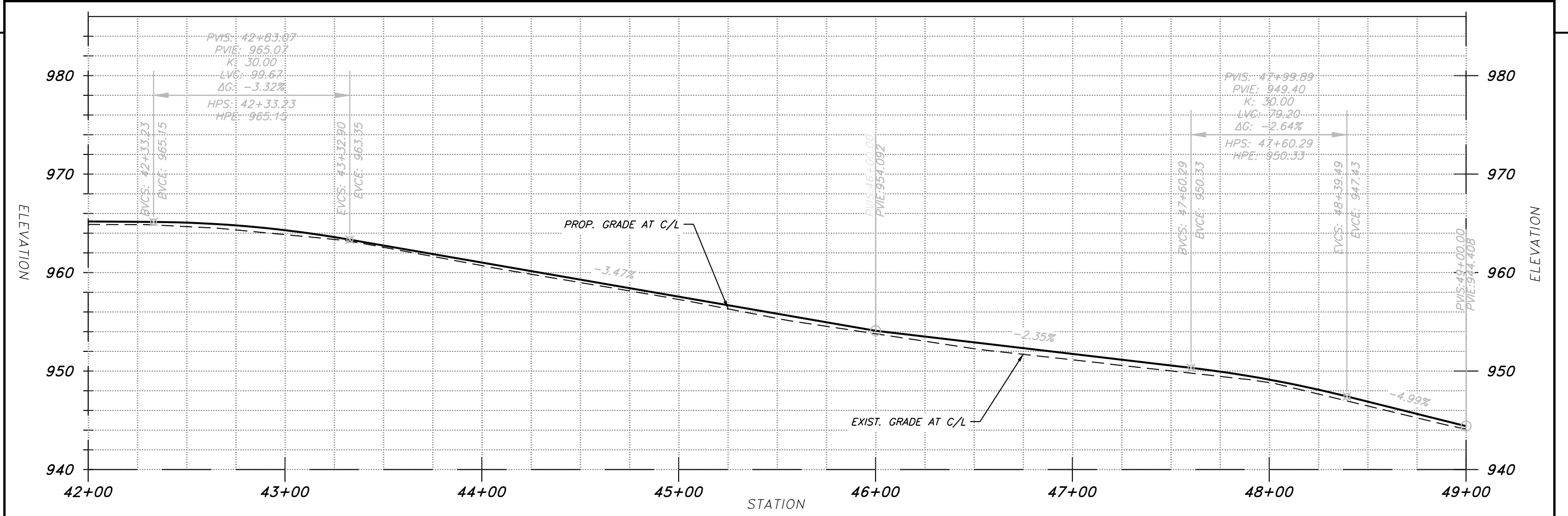
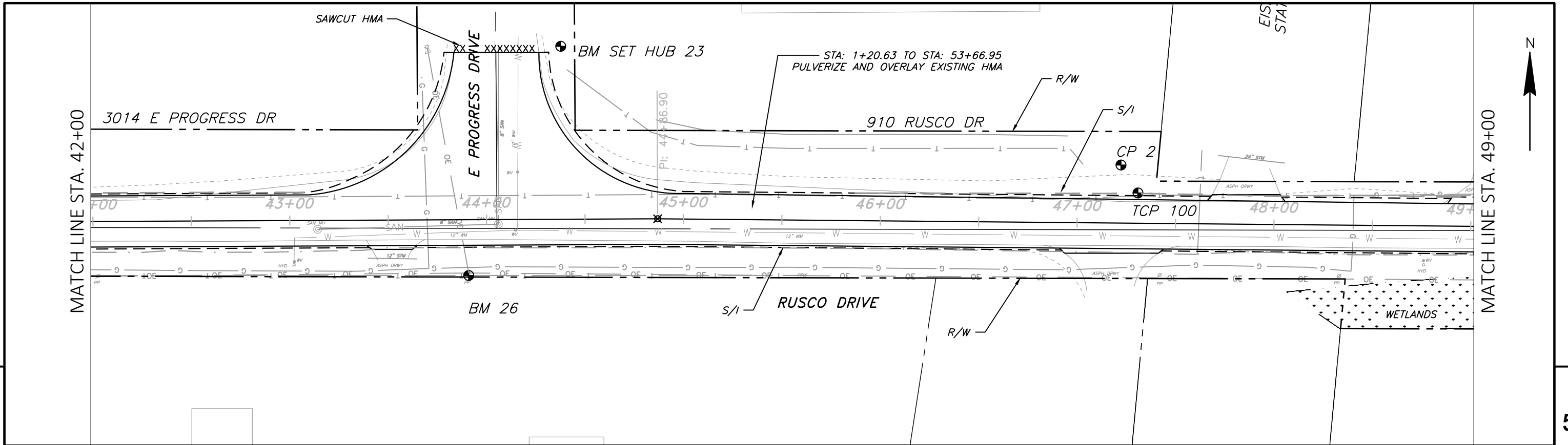
| | | | | | |
|------------------------|-------------------------------------|---------------------------|-------------------------------|-----------|---|
| PROJECT NO: 2707-03-74 | HWY: RUSCO DRIVE, CITY OF WEST BEND | COUNTY: WASHINGTON COUNTY | PLAN AND PROFILE: RUSCO DRIVE | SHEET PP4 | E |
|------------------------|-------------------------------------|---------------------------|-------------------------------|-----------|---|



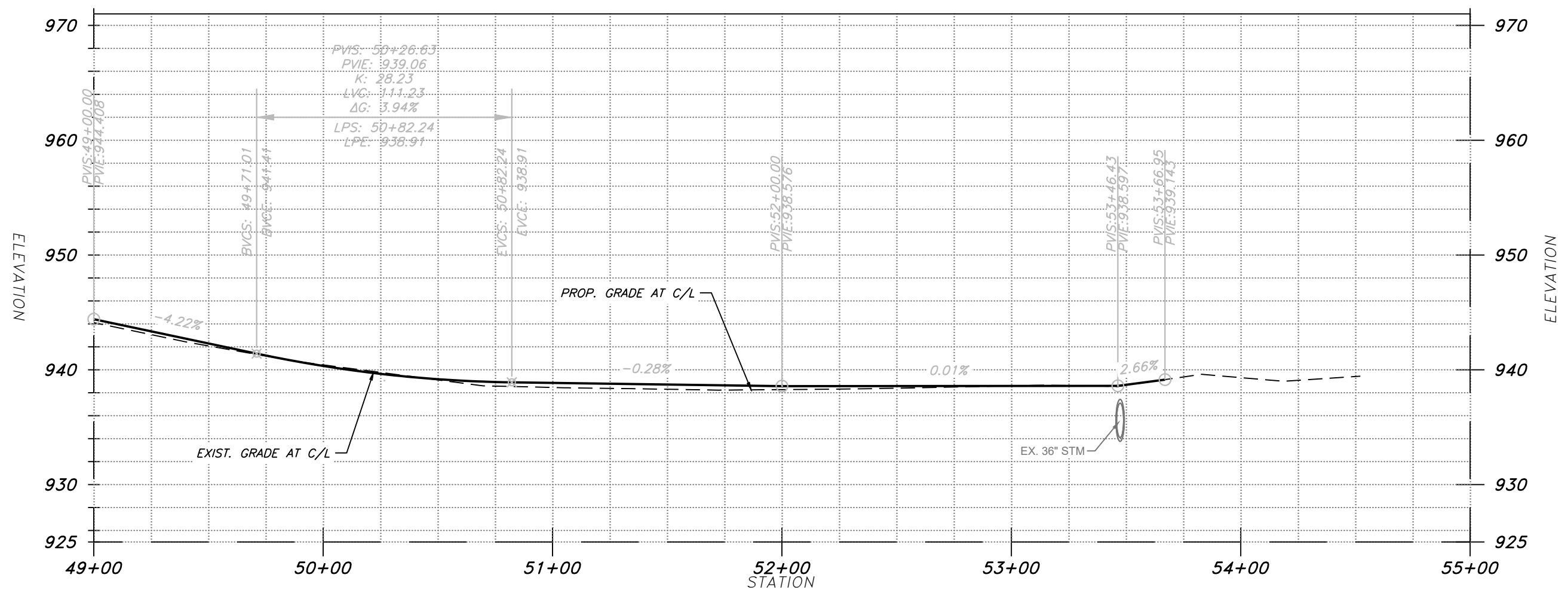
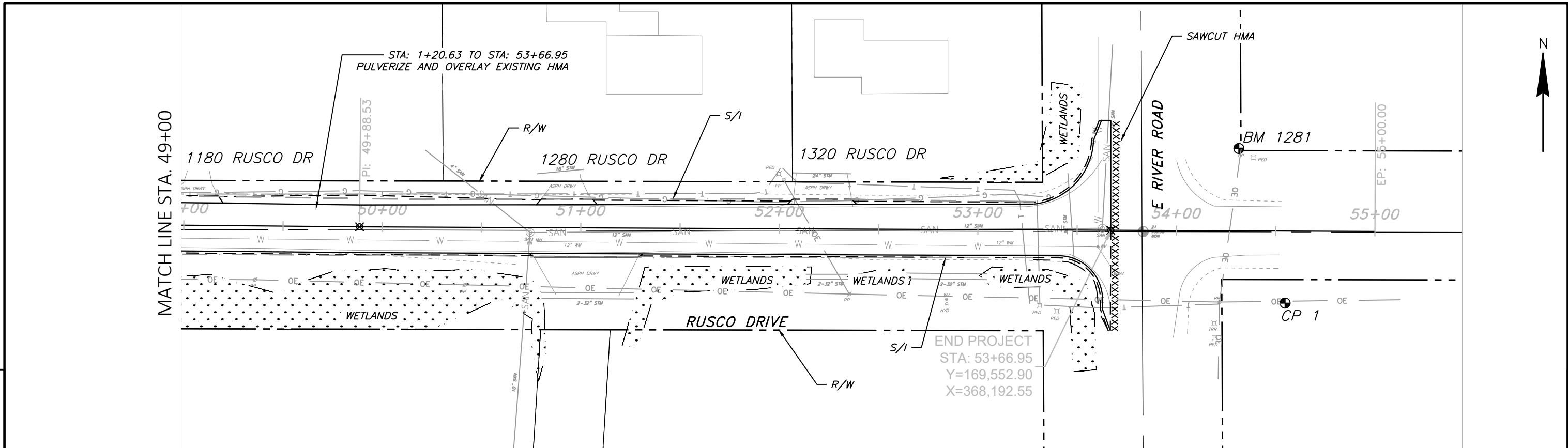
| | | | | | |
|------------------------|-------------------------------------|---------------------------|-------------------------------|-----------|---|
| PROJECT NO: 2707-03-74 | HWY: RUSCO DRIVE, CITY OF WEST BEND | COUNTY: WASHINGTON COUNTY | PLAN AND PROFILE: RUSCO DRIVE | SHEET PP5 | E |
|------------------------|-------------------------------------|---------------------------|-------------------------------|-----------|---|



| | | | | | |
|------------------------|-------------------------------------|---------------------------|-------------------------------|-----------|---|
| PROJECT NO: 2707-03-74 | HWY: RUSCO DRIVE, CITY OF WEST BEND | COUNTY: WASHINGTON COUNTY | PLAN AND PROFILE: RUSCO DRIVE | SHEET PP6 | E |
|------------------------|-------------------------------------|---------------------------|-------------------------------|-----------|---|



| | | | | | |
|------------------------|-------------------------------------|---------------------------|-------------------------------|-----------|---|
| PROJECT NO: 2707-03-74 | HWY: RUSCO DRIVE, CITY OF WEST BEND | COUNTY: WASHINGTON COUNTY | PLAN AND PROFILE: RUSCO DRIVE | SHEET PP7 | E |
|------------------------|-------------------------------------|---------------------------|-------------------------------|-----------|---|

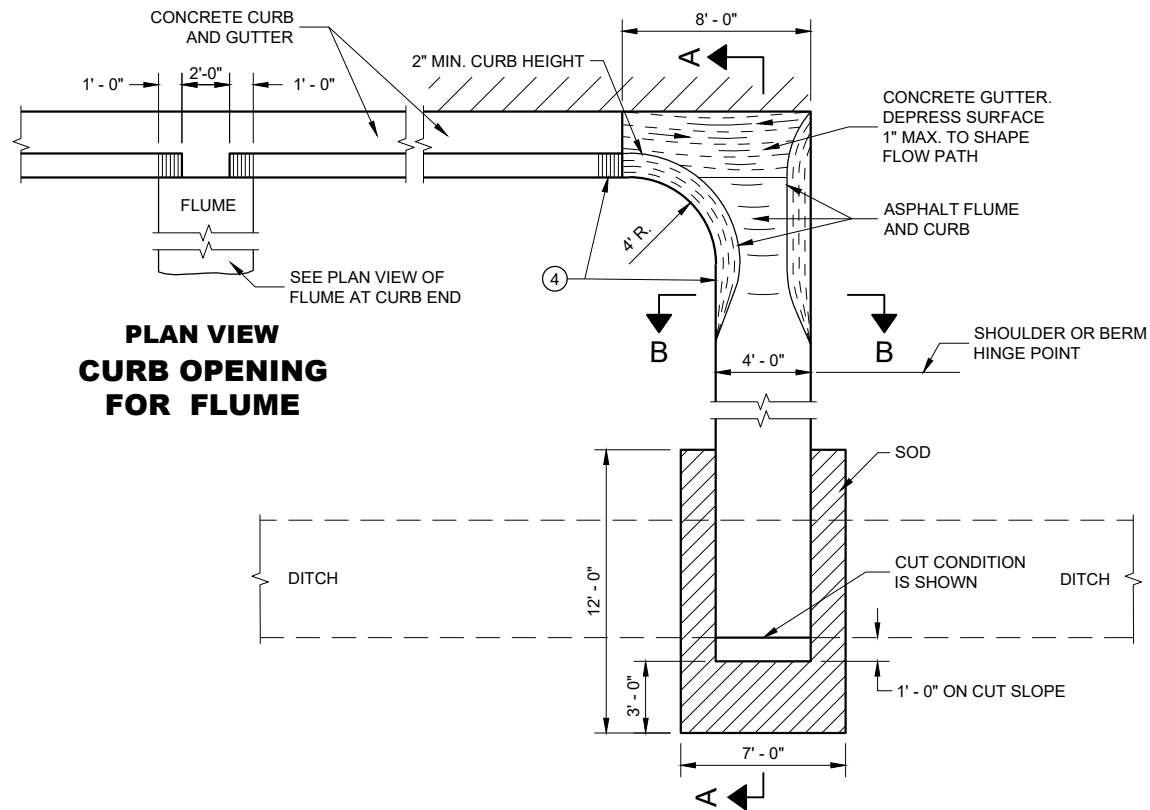


Standard Detail Drawing List

| | |
|-----------|--|
| 08D04-06 | CONCRETE SURFACE DRAINS & ASPHALTIC FLUMES |
| 08D21-01 | DRIVEWAYS WITHOUT CURB & GUTTER |
| 08D22-01 | DRIVEWAYS WITHOUT CURB & GUTTER RESURFACING PROJECTS RURAL |
| 08E09-06 | SILT FENCE |
| 15C02-08A | BARRICADES AND SIGNS FOR MAINLINE CLOSURES |
| 15C02-08B | BARRICADES AND SIGNS FOR VARIOUS CLOSURES |
| 15C02-08C | DETOUR SIGNING FOR MAINLINE CLOSURES |
| 15C03-05 | BARRICADES AND SIGNS FOR SIDEROAD CLOSURES |
| 15C08-20A | LONGITUDINAL MARKING (MAINLINE) |
| 15C12-09A | TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION |
| 15C33-04 | STOP LINE AND CROSSWALK PAVEMENT MARKING |

NOTE: TAPER CURB ENDS TO GUTTER IN 1' - 0"

ASPHALTIC FLUME



**PLAN VIEW
CURB OPENING
FOR FLUME**

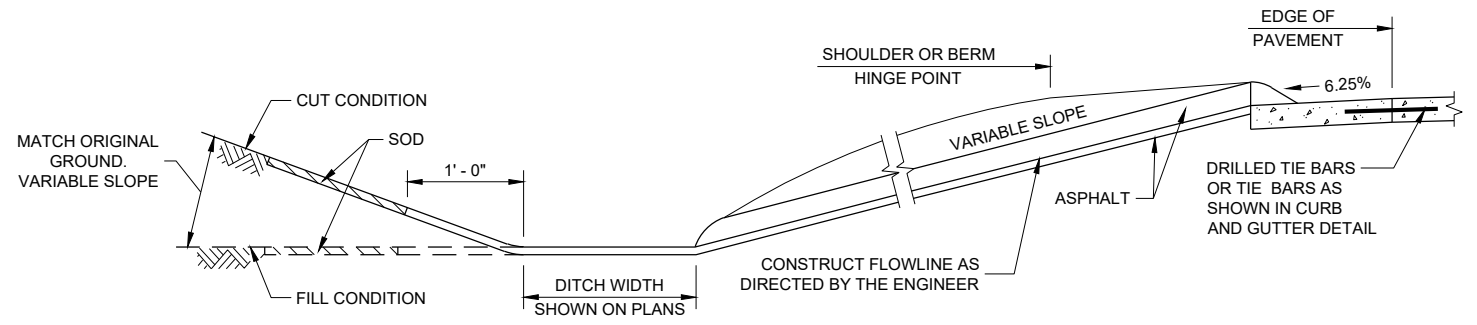
**PLAN VIEW
FLUME AT CURB END**

GENERAL NOTES

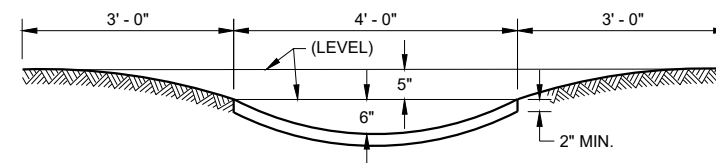
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

4" X 4" - W3.0 X W3.0 CONCRETE REINFORCEMENT SHALL BE IN ACCORDANCE WITH AASHTO SPECIFICATION M55.

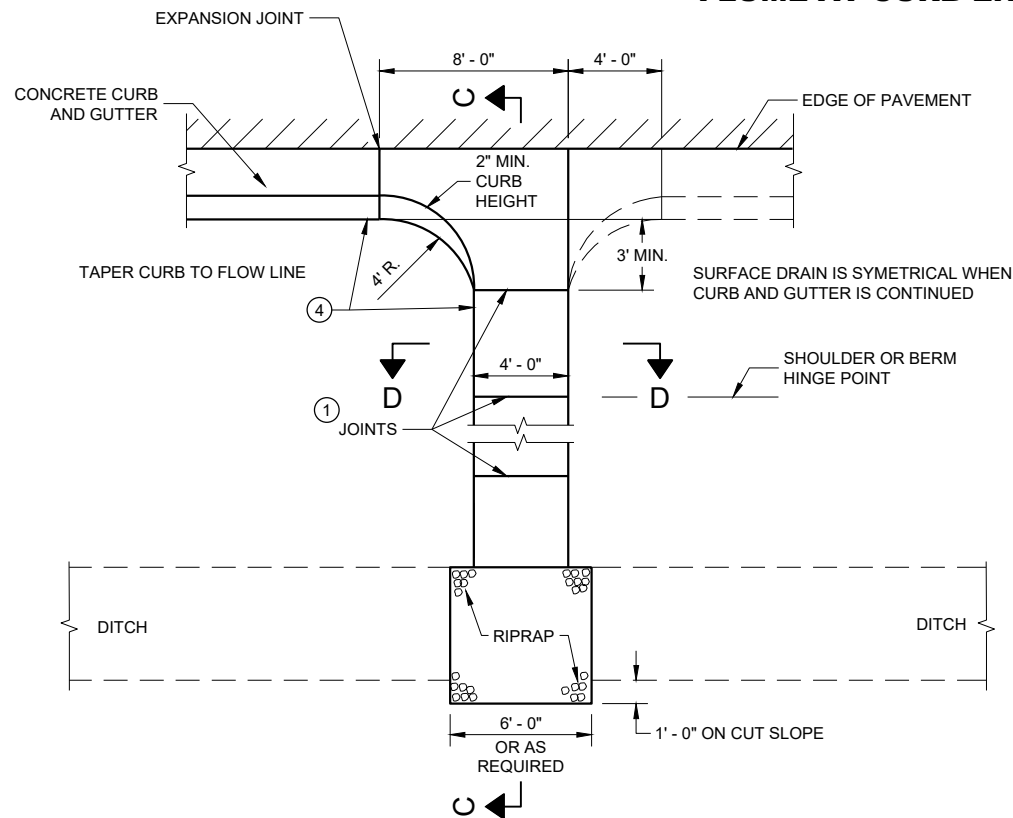
- ① JOINTS SHALL BE 1/8" TO 1/4" WIDE BY 1 1/2" DEEP AND SPACED AT UNIFORM INTERVALS OF APPROXIMATELY 4 FEET.
- ② GEOTEXTILE TYPE "R" SHALL UNDERLAY THE FULL LENGTH AND WIDTH OF THE CONCRETE SURFACE DRAIN AND RIPRAP.
- ③ CONCRETE SURFACE DRAIN WITHOUT CURB AND GUTTER MAY BE USED ON BACKSLOPES WHEN SPECIFIED.
- ④ ANGLE OF FLUME IN RELATION TO BACK OF CURB TO BE CONSTRUCTED PER THE PLAN DETAILS OR AS DIRECTED BY THE ENGINEER. ANGLE OF FLUME MAY BE OTHER THAN 90 DEGREES AS SHOWN.



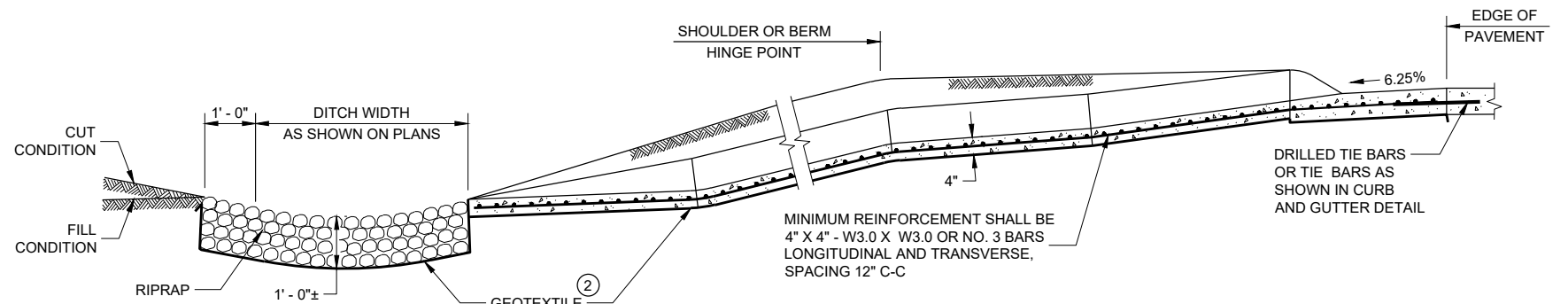
SECTION A - A



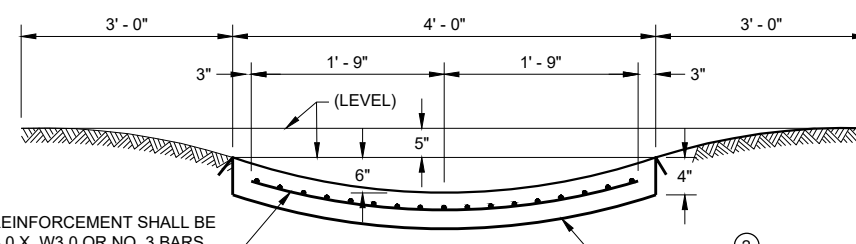
SECTION B - B



**PLAN VIEW
CONCRETE SURFACE DRAIN**



SECTION C - C



SECTION D - D

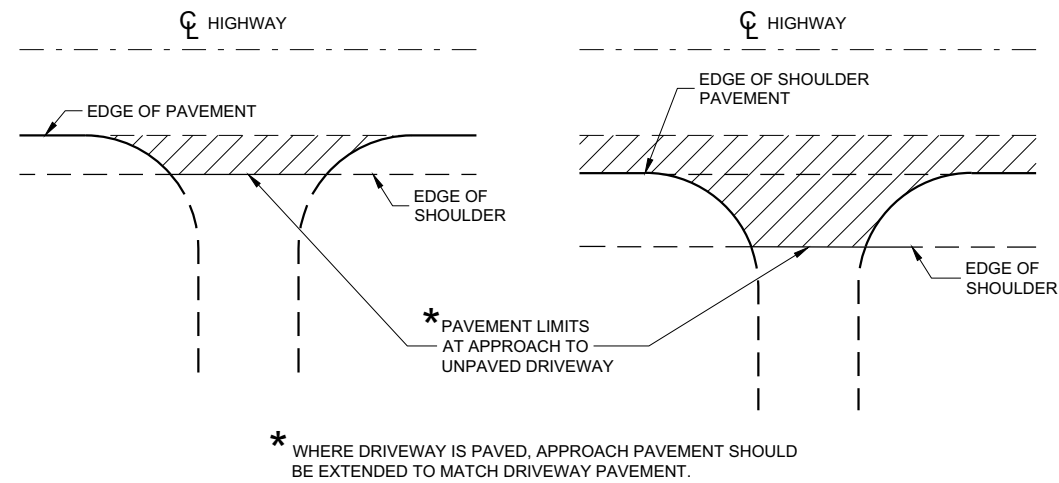
MINIMUM REINFORCEMENT SHALL BE 4" X 4" - W3.0 X W3.0 OR NO. 3 BARS LONGITUDINAL AND TRANSVERSE, SPACING 12" C-C

CONCRETE SURFACE DRAINS AND ASPHALTIC FLUMES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2021 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
ENGINEER

FHWA



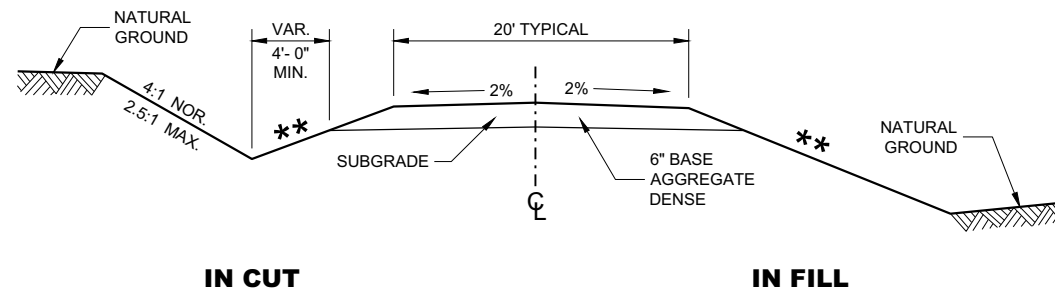
PLAN VIEW

(UNPAVED SHOULDER ON HIGHWAY)

PLAN VIEW

(PAVED SHOULDER ON HIGHWAY)

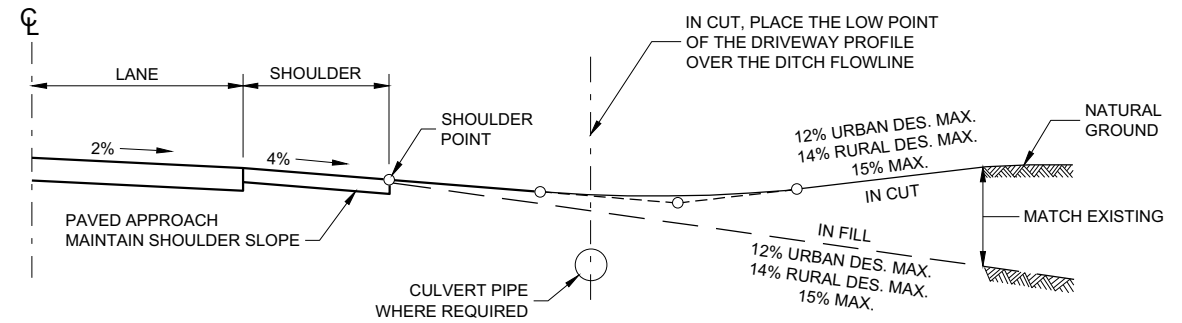
**RURAL DRIVEWAY INTERSECTION DETAIL
(NO CURB AND GUTTER OR SIDEWALK)**



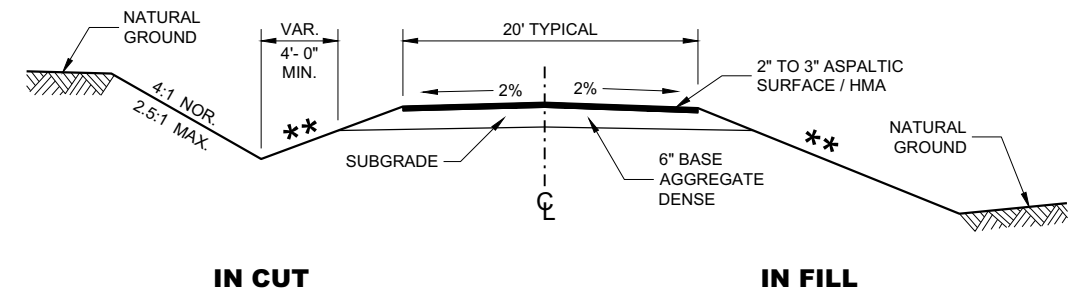
**TYPICAL CROSS SECTION FOR
PRIVATE DRIVE OR FIELD ENTRANCE
AGGREGATE SURFACE**

** SLOPE CAN VARY WITH SPEED. SEE 11-45-30.6.2

| POSTED SPEED MPH | MAX. SLOPE |
|------------------|------------|
| <35 | 4:1 |
| ≥ 35 TO < 60 | 6:1 |
| ≥60 | 10:1 |



TYPICAL DRIVEWAY PROFILES

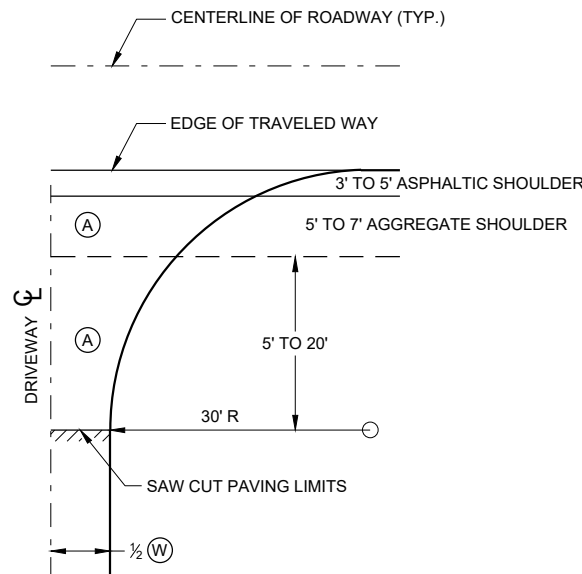


**TYPICAL CROSS SECTION FOR
PRIVATE DRIVE OR FIELD ENTRANCE
ASPHALTIC SURFACE**

| | |
|--|---|
| DRIVEWAYS WITHOUT CURB AND GUTTER | |
| STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION | |
| APPROVED December 2017 DATE | /s/ Rodney Taylor ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR |
| FHWA | |

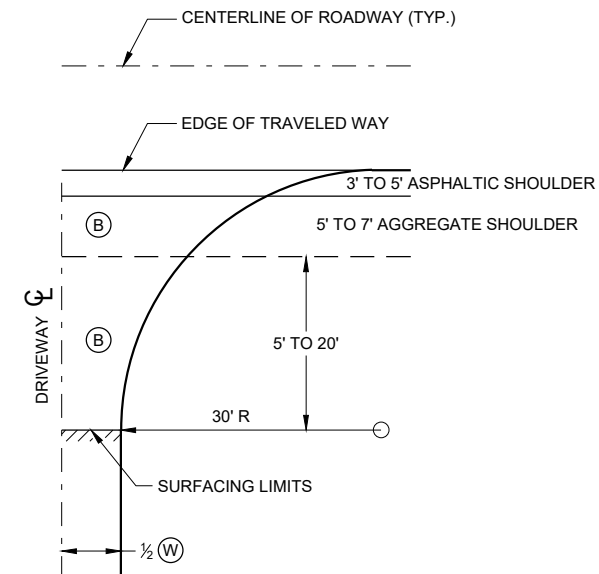
GENERAL NOTES

- ① DESIGN WILL DETERMINE FINAL DRIVEWAY ASPHALTIC THICKNESS BASED ON TYPE OF USAGE AND LOADINGS.

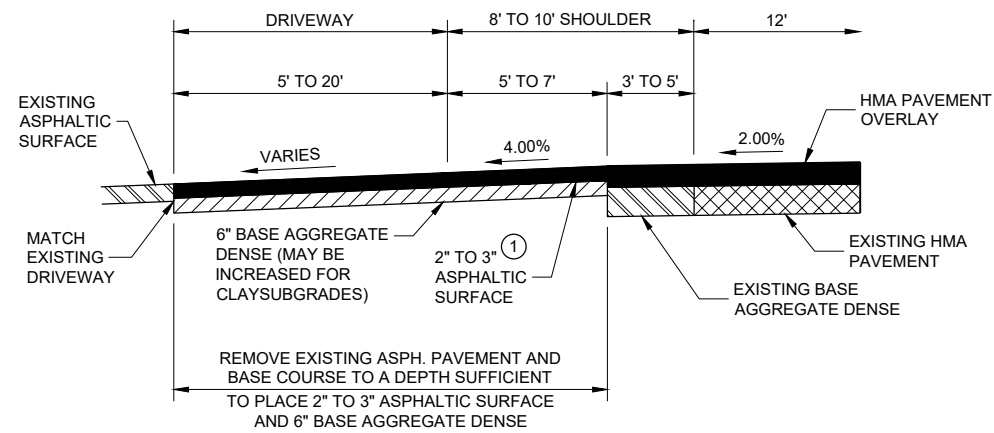


- Ⓐ : PAID FOR AS ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES. (TON)
- Ⓑ : PAID FOR AS BASE AGGREGATE DENSE 1 1/4" (TON)
- ⒲ : DRIVEWAY WIDTH 16' MIN. - 24' MAX.

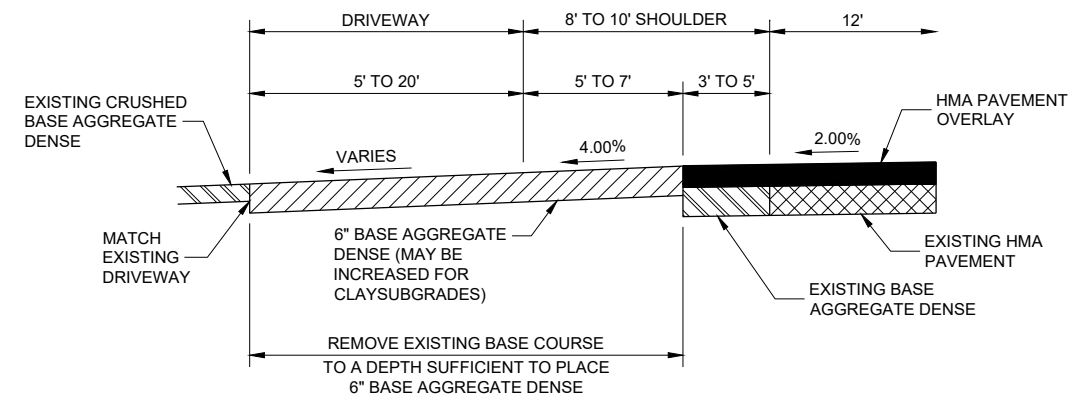
**PLAN VIEW
HALF SECTION**



**PLAN VIEW
HALF SECTION**



**PROFILE VIEW
RURAL ENTRANCE
WITH ASPHALTIC SURFACE
RESURFACING PROJECTS**



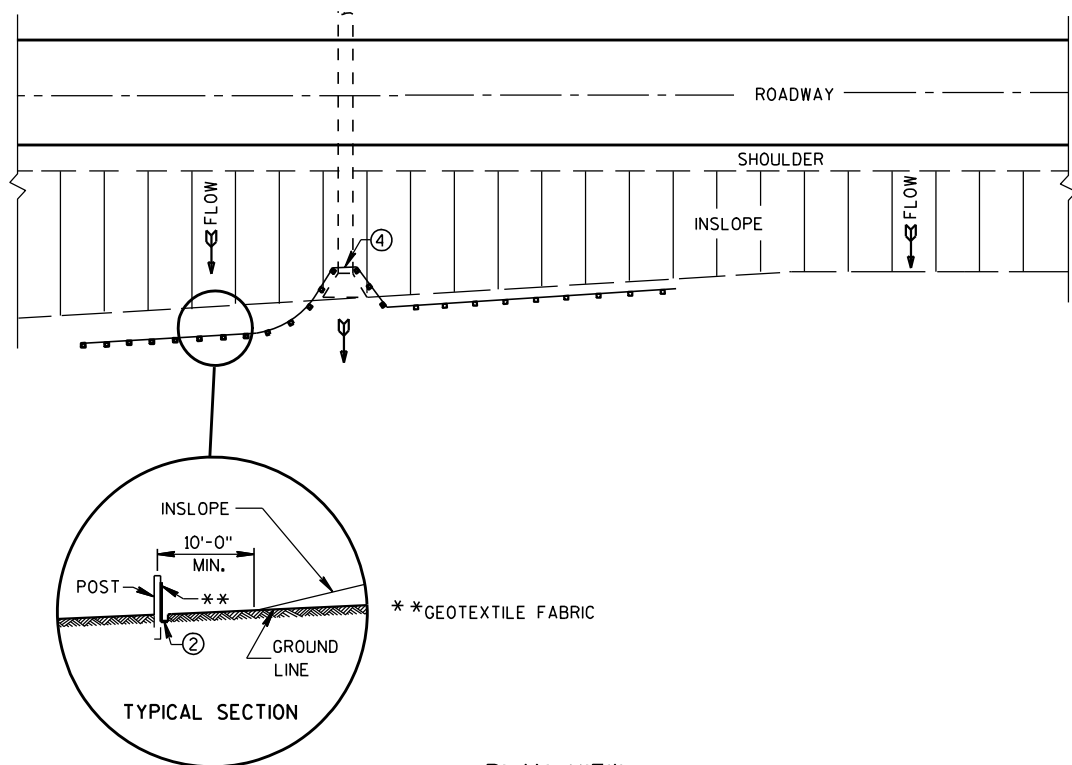
**PROFILE VIEW
RURAL ENTRANCE
WITH AGGREGATE SURFACE
6" BASE AGGREGATE DENSE
RESURFACING PROJECTS**

**DRIVEWAYS WITHOUT CURB
AND GUTTER RESURFACING
PROJECTS RURAL**

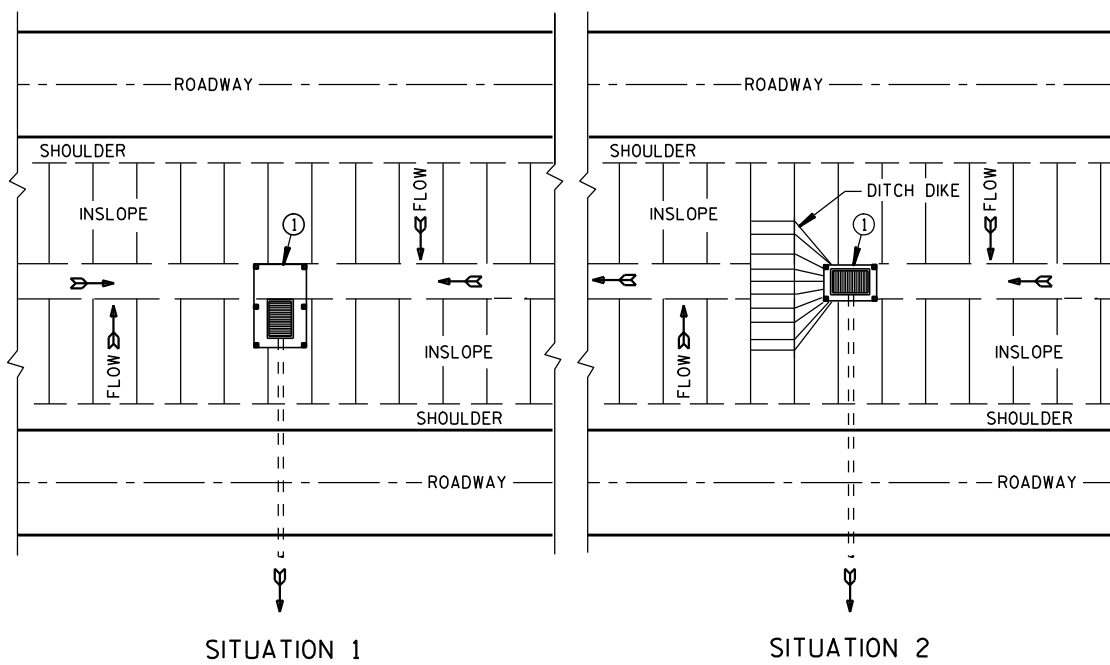
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
December 2016 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
ENGINEER

FHWA



PLAN VIEW
TYPICAL APPLICATION OF SILT FENCE

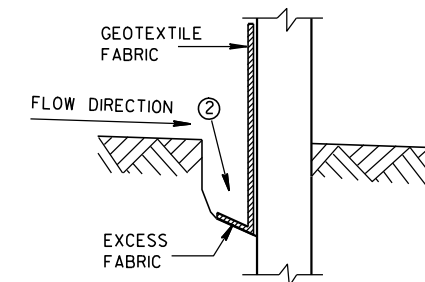


SITUATION 1 SITUATION 2
PLAN VIEW
SILT FENCE AT MEDIAN SURFACE DRAINS

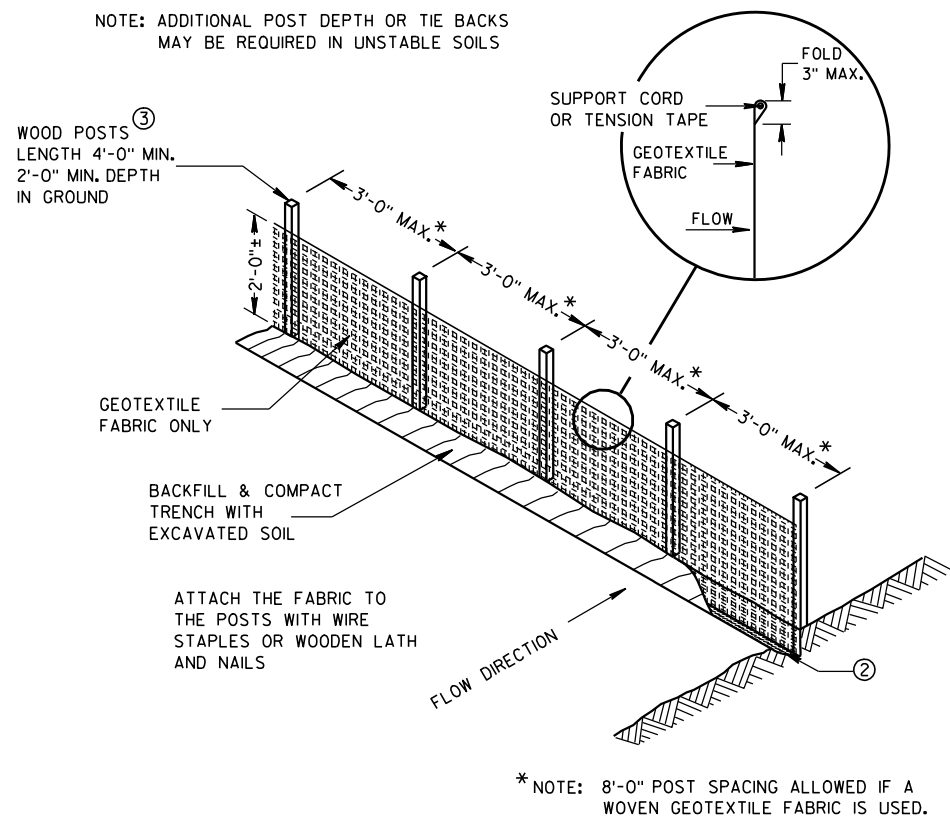
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

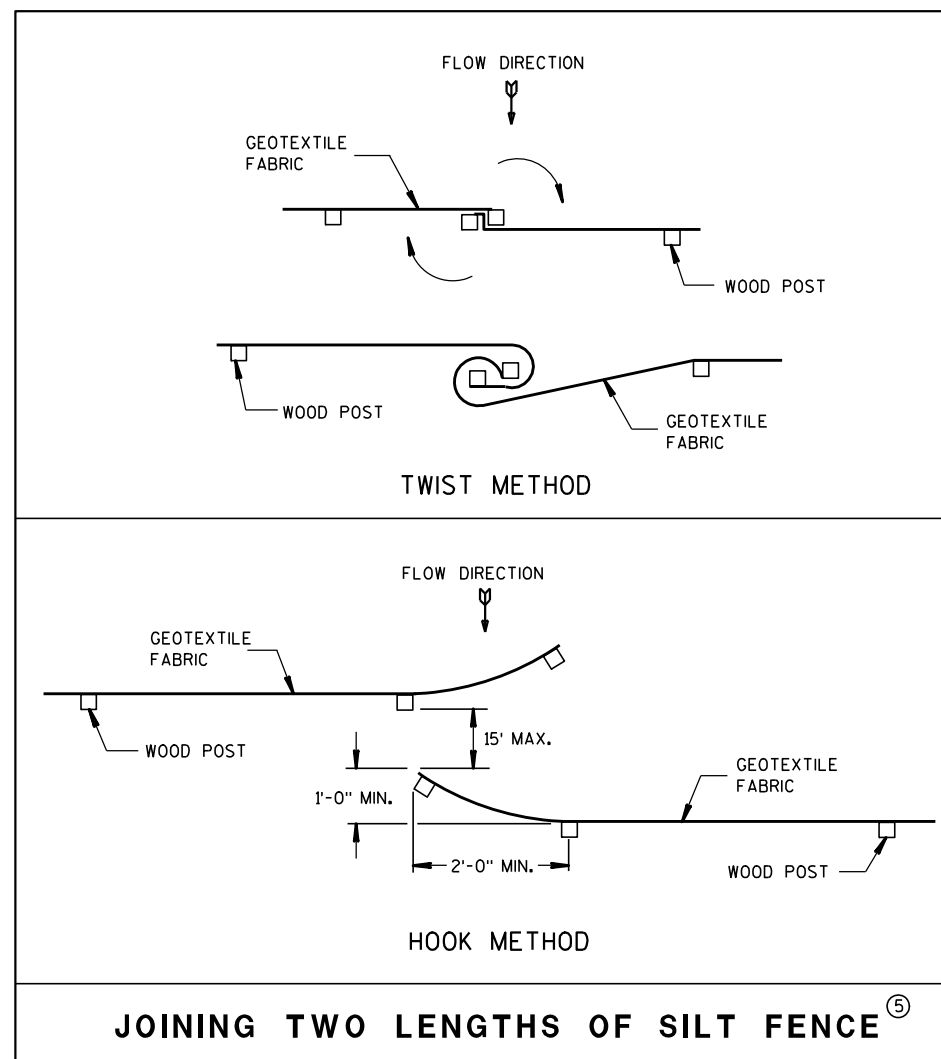
- ① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- ③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1 1/8" X 1 1/8" OF OAK OR HICKORY.
- ④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- ⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



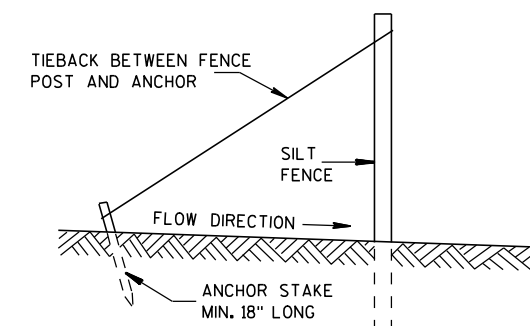
TRENCH DETAIL



SILT FENCE



JOINING TWO LENGTHS OF SILT FENCE ⑤

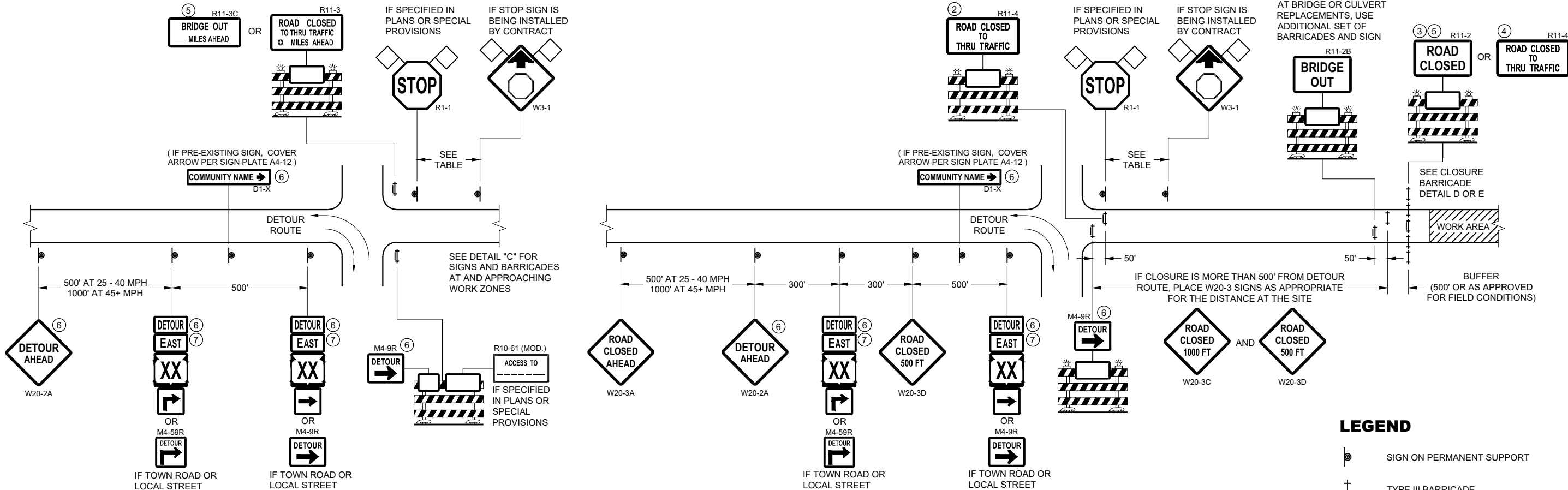


SILT FENCE TIE BACK
(WHEN REQUIRED BY THE ENGINEER)

SILT FENCE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
4-29-05 /S/ Beth Canestra
DATE CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA



**DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR**

WORK ZONE GREATER THAN OR EQUAL TO 1/2 MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)

**DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR**

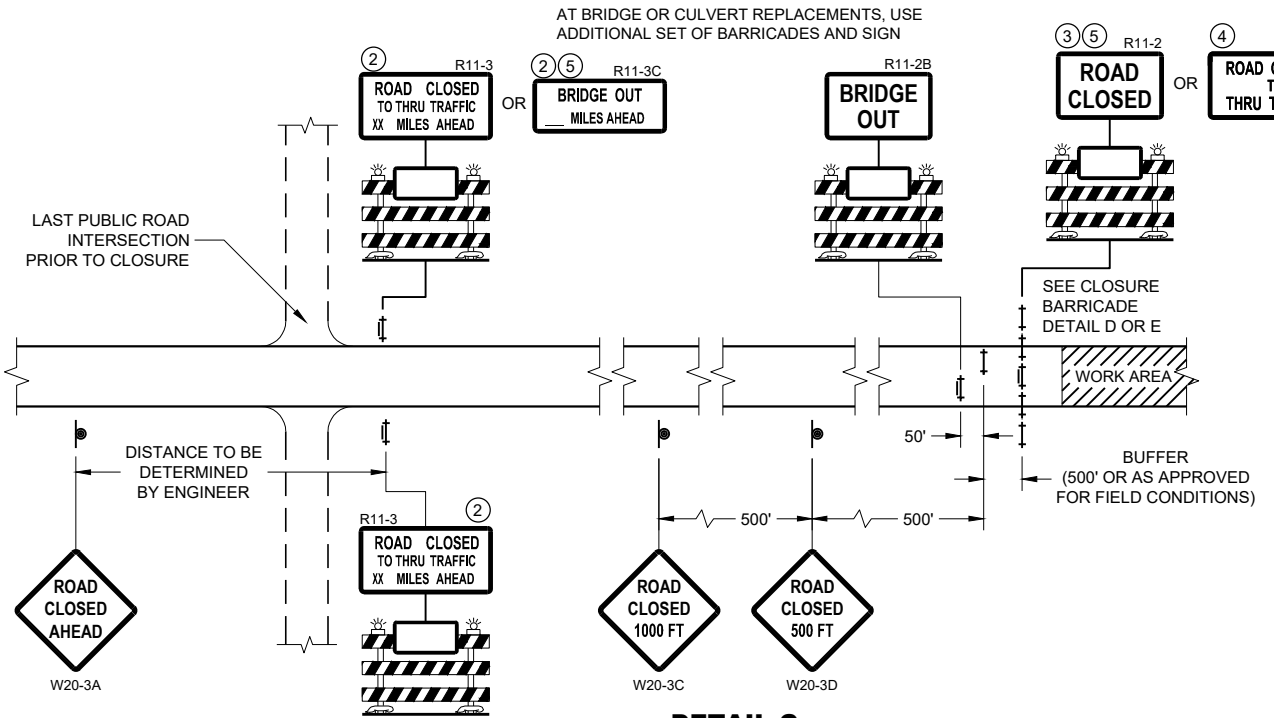
WORK ZONE LESS THAN 1/2 MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
- FLAGS, 16" X 16" MIN. (ORANGE)

| SPEED LIMIT (MPH) | "STOP AHEAD" ADVANCE WARNING DISTANCE (FT) |
|-------------------|--|
| 25 | 200 |
| 30 | 200 |
| 35 | 350 |
| 40 | 350 |
| 45 | 500 |
| 50 | 550 |
| 55 | 750 |

- M4 - 8
- M3 - X
- M1 - 4
- M1 - 6
- M1 - 5A
- M05 - 1
- M06 - 1



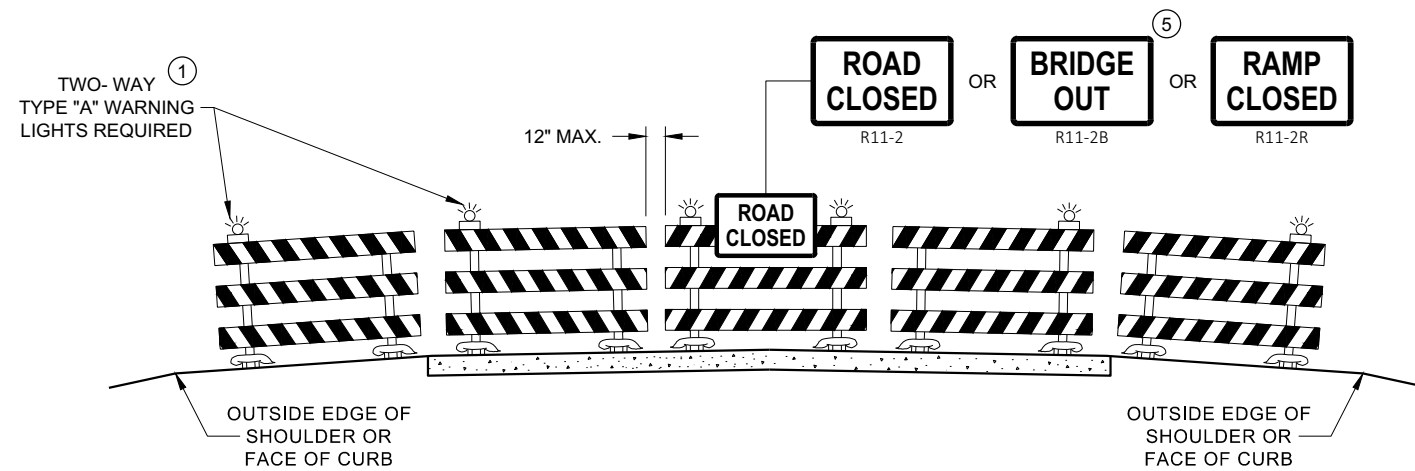
**DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR**

SEE SDD 15C2-SHEET "b"
FOR GENERAL NOTES
AND FOOTNOTES ① THROUGH ⑦

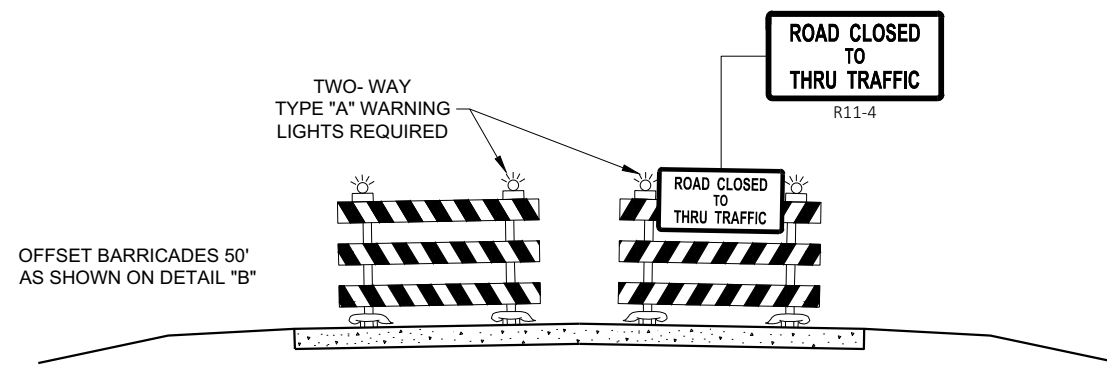
**BARRICADES AND SIGNS
FOR MAINLINE CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020 /S/ Andrew Heidtke
DATE DATE WORK ZONE ENGINEER
FHWA



**DETAIL D
ROAD CLOSURE BARRICADE DETAIL
APPROACH VIEW**



**DETAIL E
LANE CLOSURE BARRICADE DETAIL
APPROACH VIEW**

SEE SDD 15C2 - SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW - INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11 - 2, R11 - 3, M4 - 9, R11 - 4, AND R10 - 61 SIGNS PLACED ON THE BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE RAIL OR BOTTOM RAILS.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

- R11 - 2 SHALL BE 48" X 30"
- R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60" X 30"
- M4 - 9 SHALL BE 30" X 24"
- M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1 - 1 SHALL BE 36" X 36"

- ① TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT SPACING).
- ② THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.
- ③ FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".
- ④ FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".
- ⑤ FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.
- ⑥ INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.
- ⑦ "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

**BARRICADES AND SIGNS
FOR
VARIOUS CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

LEGEND

- SIGN ON PERMANENT SUPPORT
- WORK AREA
- M4 - 8
- M3 - X
- M1 - 4
- M1 - 6
- M1 - 5A
- M05 - 1
- M06 - 1
- M06 - 1

GENERAL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. MODIFY EXISTING SIGNS WHERE POSSIBLE.

THE SPACING BETWEEN TRAFFIC CONTROL AND DETOUR SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

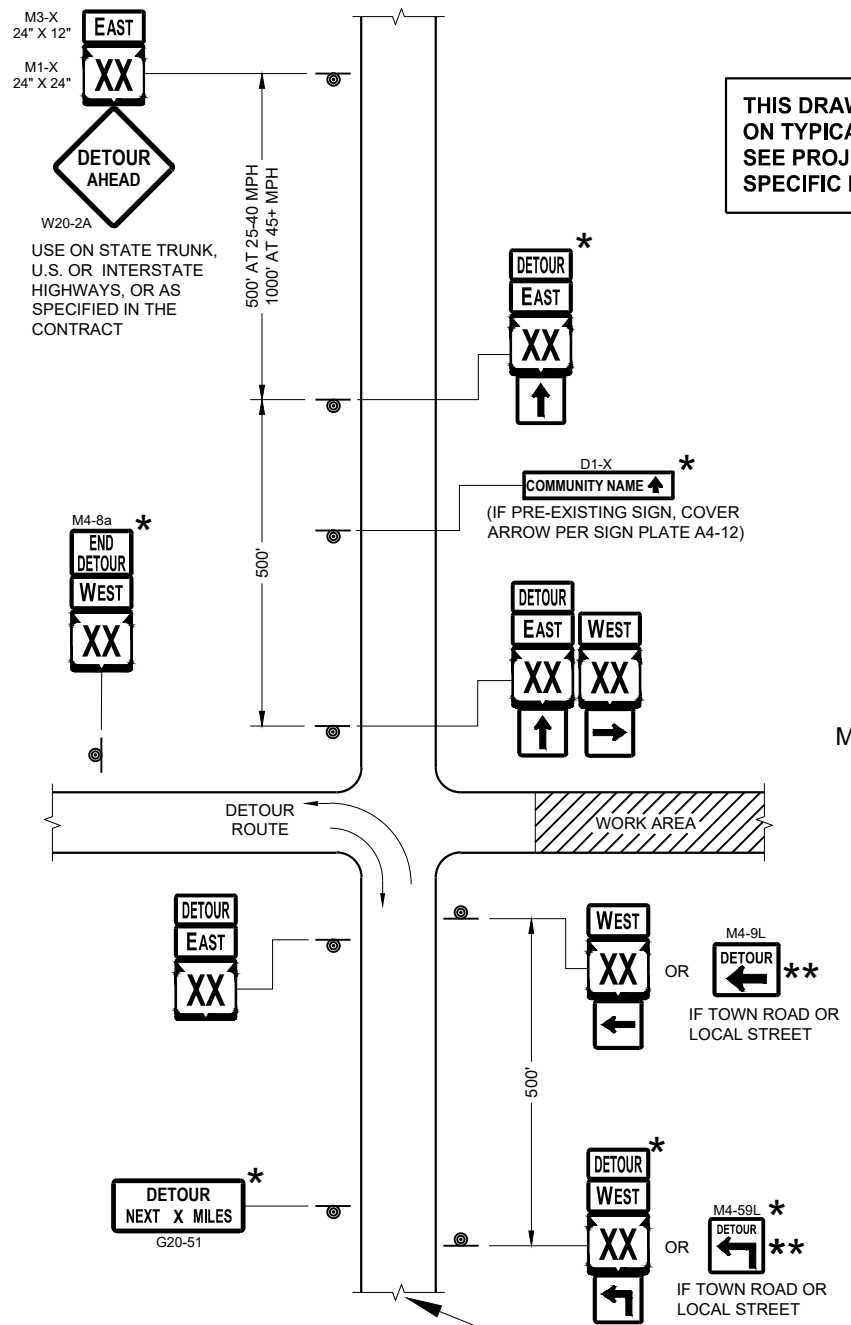
"MO" SIGNS ARE THE SAME AS "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGN SIZES SHALL BE AS FOLLOWS:

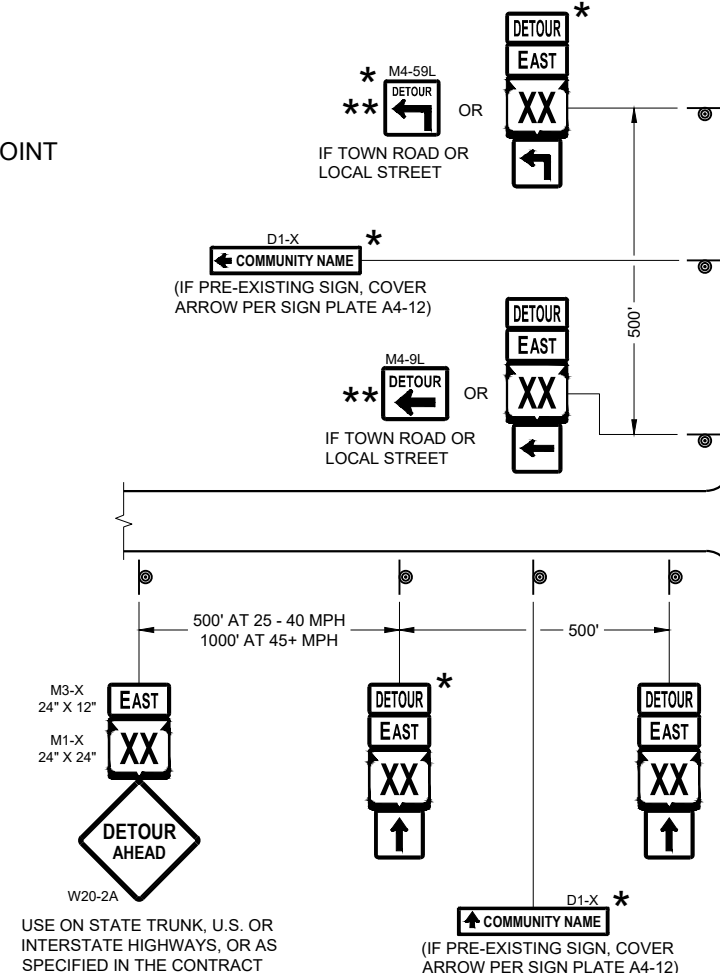
- M3-X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4-8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1-4, M1-5A AND M1-6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- M05-1 AND M06-1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- M4-9 AND M4-59 SHALL BE 30" X 24"
- M4-8a SHALL BE 24" X 18"
- G20-51 SHALL BE 60" X 24"
- W20-2A SHALL BE 48" X 48"
- D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.

- * OPTIONAL SIGNS. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS.
- ** FOR A TOWN ROAD OR LOCAL STREET DETOURED ONTO A STATE TRUNK HIGHWAY, PLACE A ROAD NAME PLAQUE ABOVE THE M4-9 SIGN AS SPECIFIED IN THE CONTRACT.

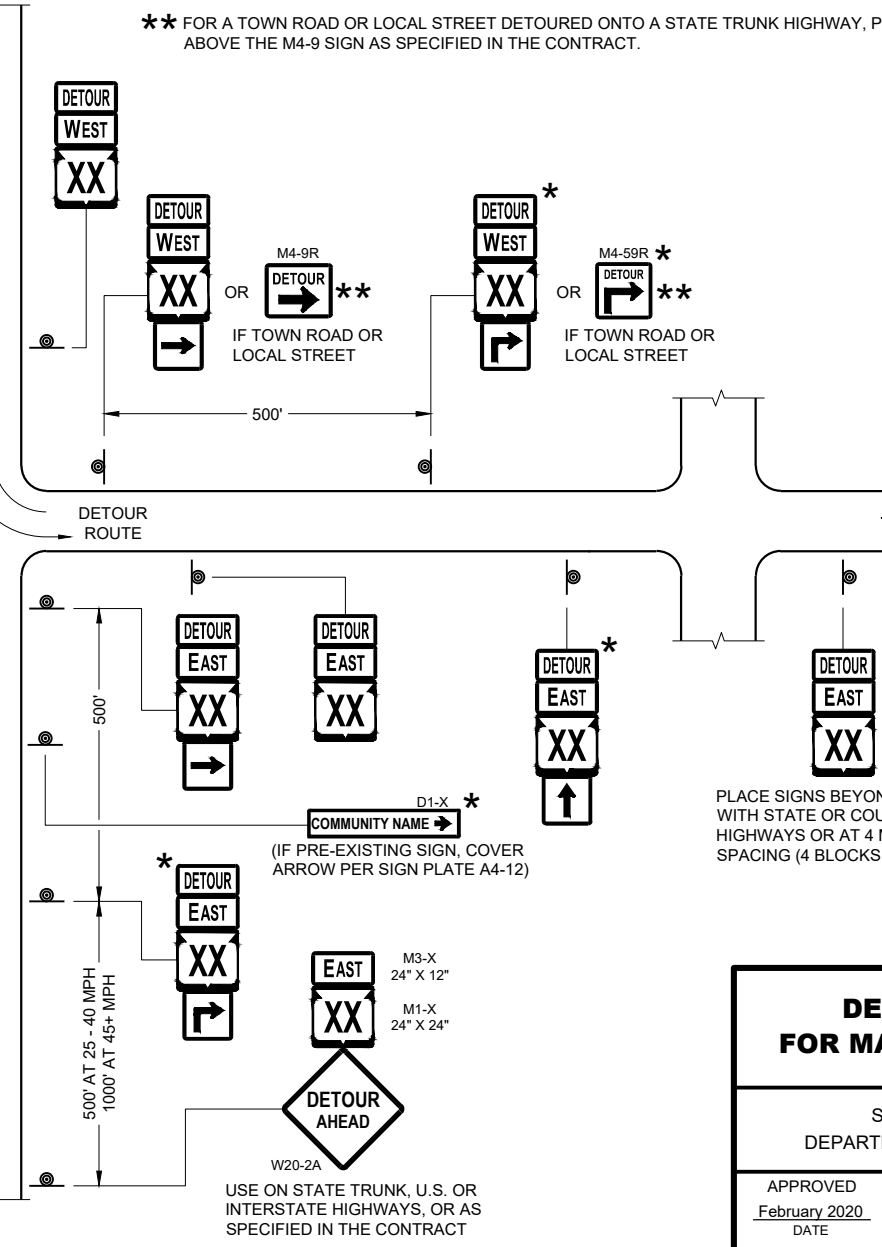
THIS DRAWING PROVIDES GENERAL GUIDANCE ON TYPICAL DETOUR SIGN LAYOUT AND SPACING. SEE PROJECT DETOUR SIGNING SHEETS FOR SPECIFIC DETAILS FOR EACH PROJECT.



MATCH POINT



DETAIL F
DETOUR SIGNING



**DETOUR SIGNING
FOR MAINLINE CLOSURES**

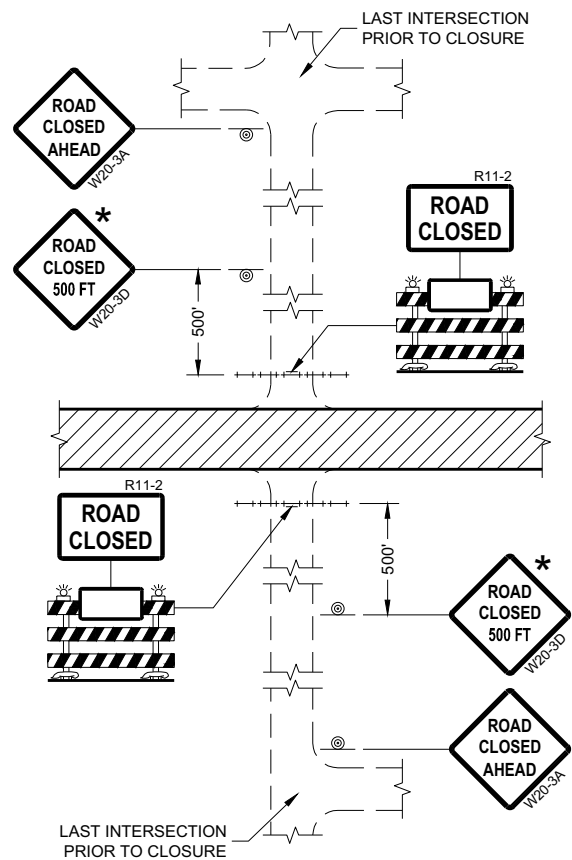
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

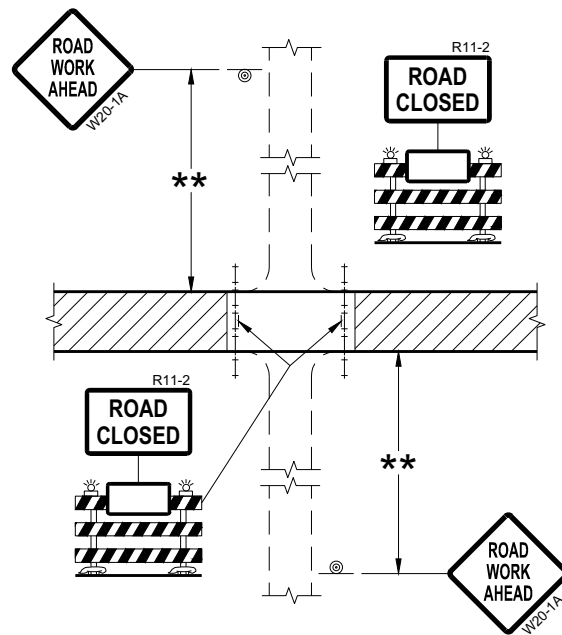
FHWA

SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS AND DETAIL A OR B ON SDD SHEET 15C02 - SHEET "a"

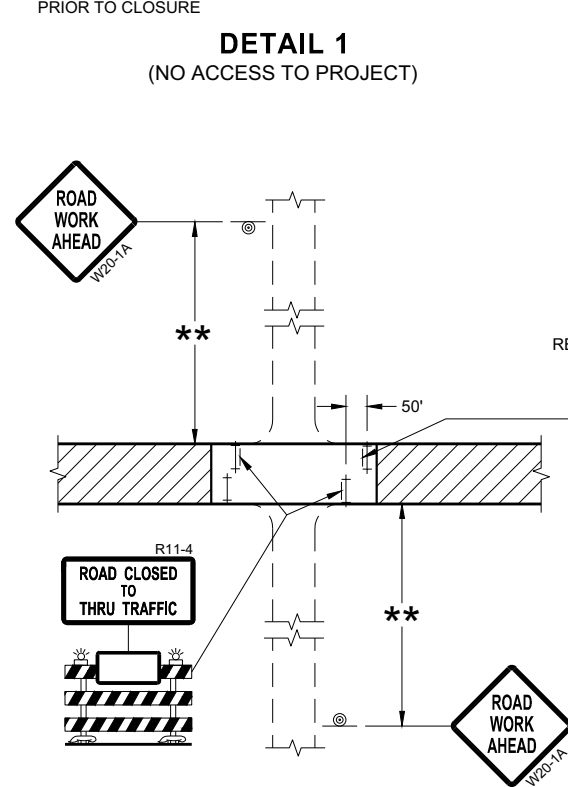
PLACE SIGNS BEYOND INTERSECTIONS WITH STATE OR COUNTY TRUNK HIGHWAYS OR AT 4 MILE MAXIMUM SPACING (4 BLOCKS IF URBAN AREA)



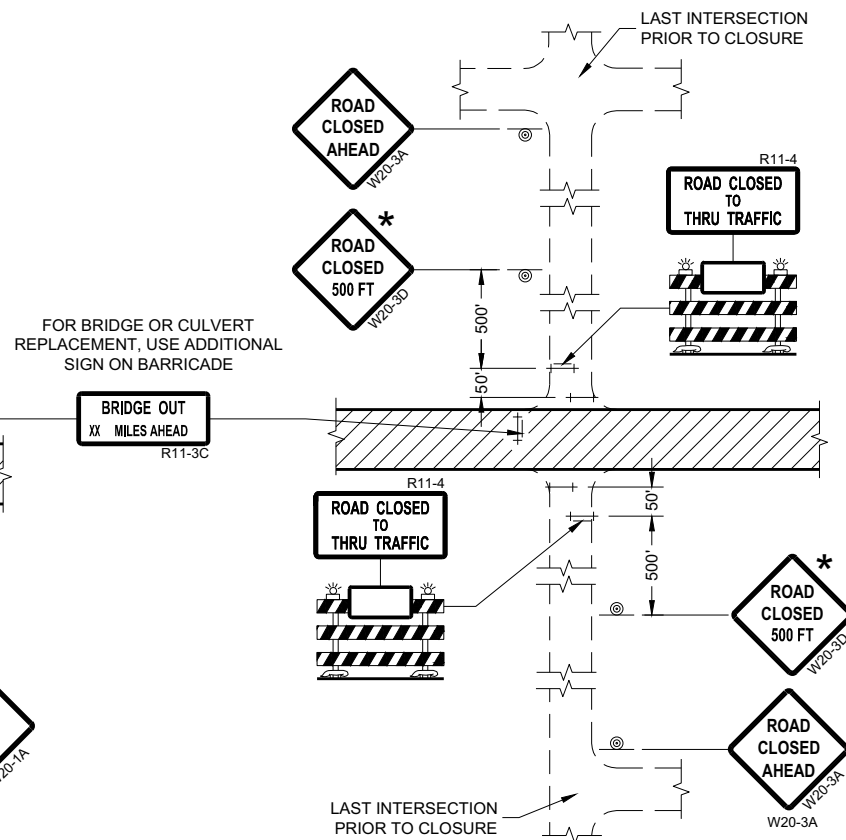
DETAIL 1
(NO ACCESS TO PROJECT)



DETAIL 2
(PUBLIC CROSS-TRAFFIC MAINTAINED.
NO ACCESS TO PROJECT)



DETAIL 3
(PUBLIC CROSS-TRAFFIC MAINTAINED.
CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)



DETAIL 4
(CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE (500 FEET DESIRABLE) TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3, AND R11-4 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
R11-2 SHALL BE 48" X 30".
R11-4 AND R11-3 SHALL BE 60" X 30".

- * OMIT THE "ROAD CLOSED 500 FT." SIGN IF THE LAST INTERSECTION IS 500 FEET OR LESS FROM THE WORK ZONE.
- ** 500' MAX. OR AT LAST INTERSECTION, WHICHEVER IS CLOSEST.

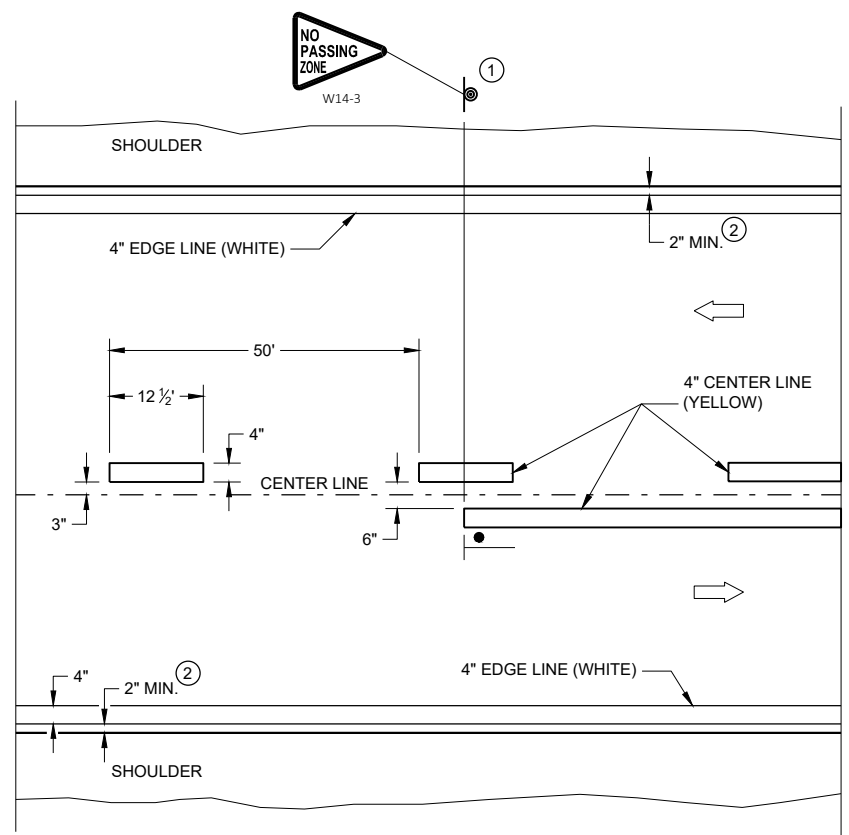
LEGEND

- ⊙ SIGN ON PERMANENT SUPPORT
- ⊥ TYPE III BARRICADE
- ⊥ TYPE III BARRICADE WITH ATTACHED SIGN
- ⚡ TYPE "A" WARNING LIGHT (FLASHING)
- ▨ WORK AREA

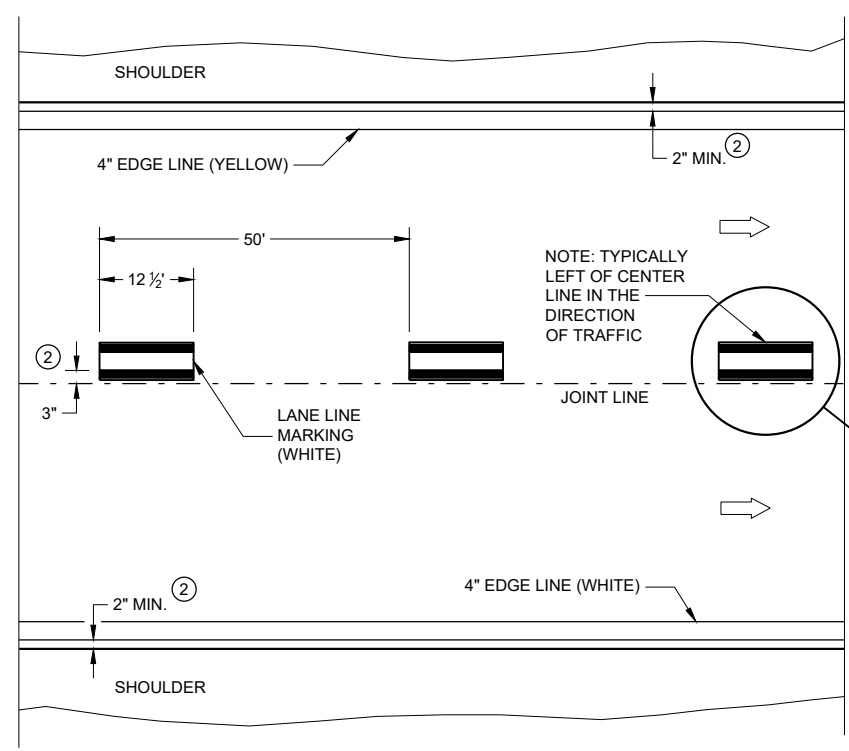
**BARRICADES AND SIGNS
FOR
SIDEROAD CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
July 2018 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

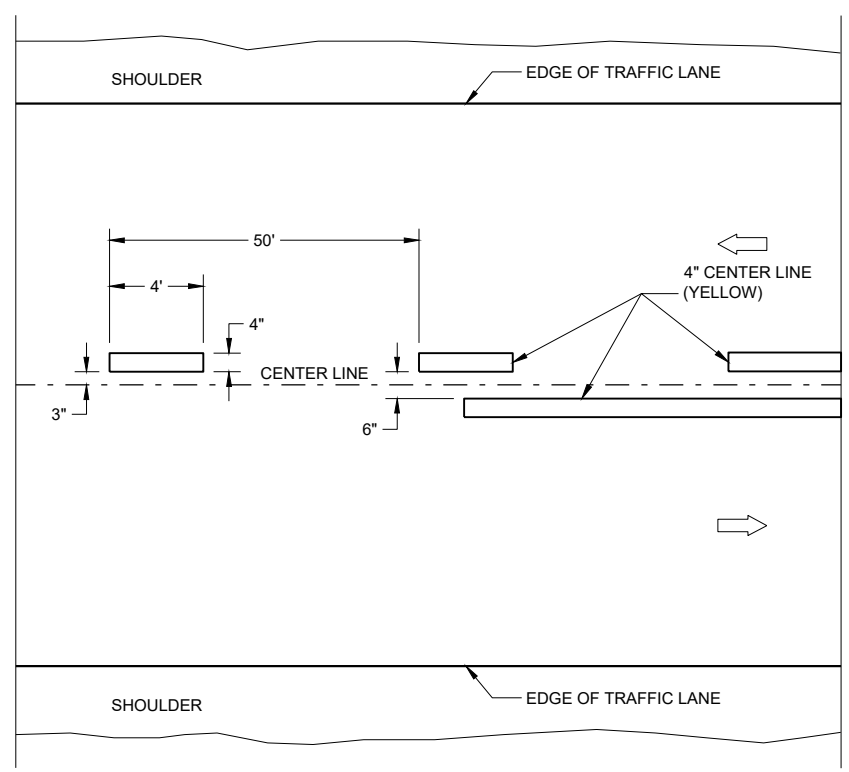


TWO WAY TRAFFIC

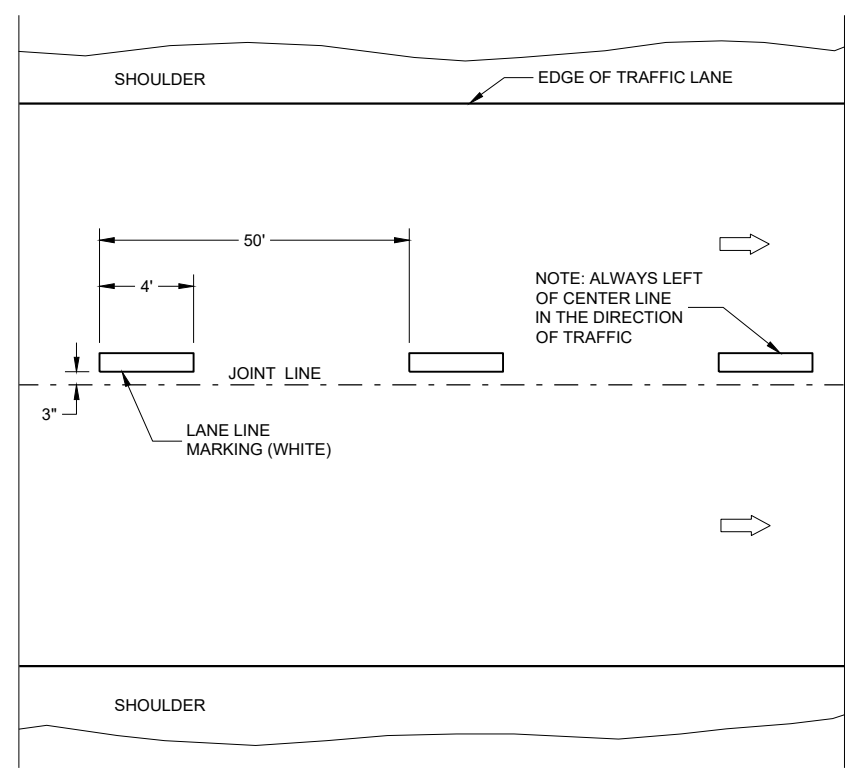


ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING



TWO WAY TRAFFIC



ONE WAY TRAFFIC

TEMPORARY PAVEMENT MARKING

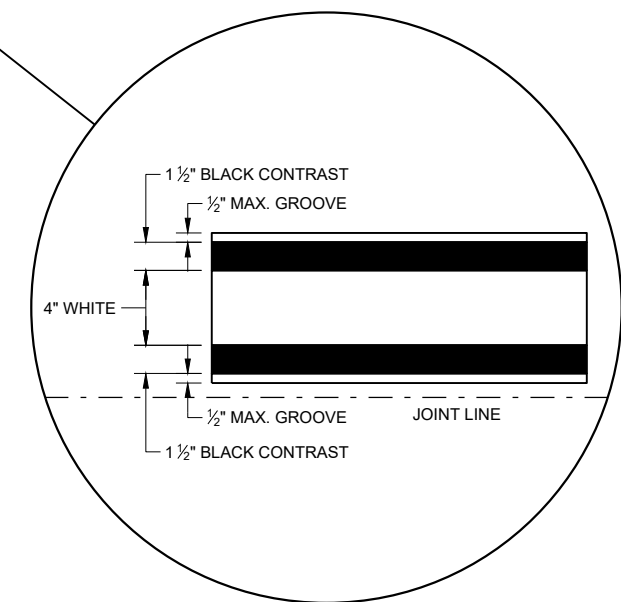
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

- ① LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING
- ② MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

LEGEND

- "T" MARKING
- SIGN ON PERMANENT SUPPORT
- DIRECTION OF TRAFFIC








LONGITUDINAL MARKING (MAINLINE)

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED
 February 2020 /S/ Matthew Rauch
 DATE STATEWIDE SIGNING AND MARKING ENGINEER
 FHWA

LEGEND

-  SIGN ON PORTABLE OR PERMANENT SUPPORT
-  TEMPORARY PORTABLE RUMBLE STRIP ARRAY
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS, DEVICES, AND LOCATION OF ALL FLAGGERS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGING

FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT REMOVE TEMPORARY PORTABLE RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.

- ① FOR MOVING WORK OPERATIONS, POST ADDITIONAL W20-7A FLAGGER SIGNS AT APPROXIMATELY 3,500' INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.
- ② SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

WHEN THE DISTANCE BETWEEN FLAGGERS EXCEEDS 2 MILES, A PILOT CAR IS REQUIRED. WHEN CURVES REDUCE SIGHT DISTANCE BELOW 400', A PILOT CAR IS REQUIRED.

TEMPORARY PORTABLE RUMBLE STRIPS

UTILIZE TEMPORARY PORTABLE RUMBLE STRIPS ON ALL FLAGGING OPERATIONS.

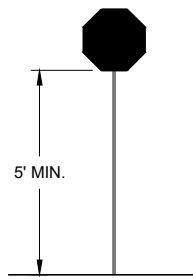
- ③ EACH TEMPORARY PORTABLE RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS PLACED TRANSVERSE ACROSS THE LANE AT THE LOCATIONS SHOWN. WITHIN EACH ARRAY, SPACING BETWEEN RUMBLE STRIPS SHALL BE 15 FEET ON CENTER

ONLY USE TEMPORARY PORTABLE RUMBLE STRIPS FROM THE APPROVED PRODUCTS LIST.

INSTALL TEMPORARY RUMBLE STRIPS PER MANUFACTURER'S RECOMMENDATIONS.

PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.

DO NOT INSTALL TEMPORARY PORTABLE RUMBLE STRIPS ON GRAVEL, MILLED SURFACES, OR ASPHALT THAT HAS BEEN PAVED LESS THAN 12 HOURS.



STOP/SLOW PADDLE ON SUPPORT STAFF

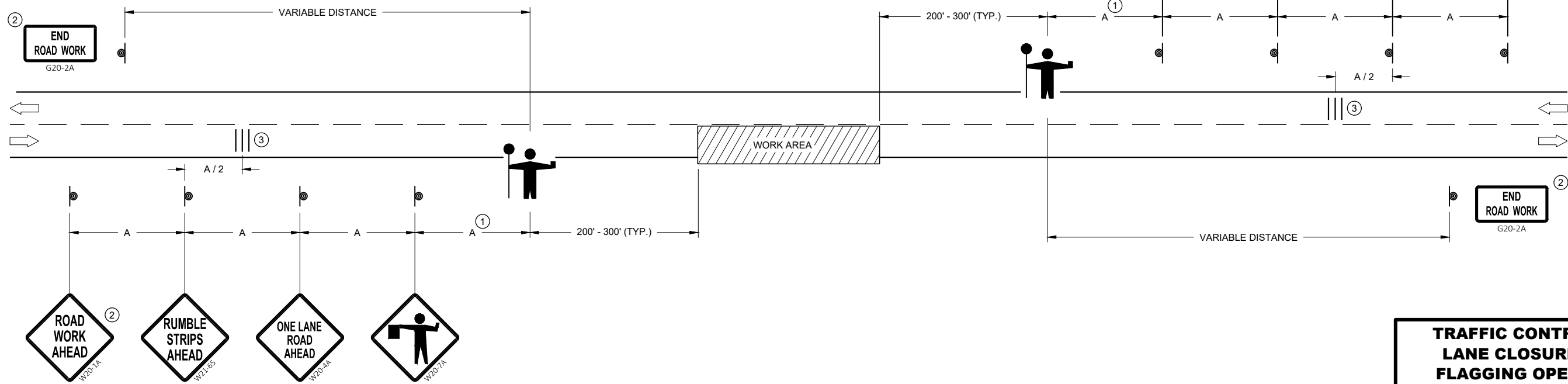
SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE

| SPEED LIMIT | SPACING "A" |
|-------------|-------------|
| 25-30 MPH | 200' |
| 35-40 MPH | 350' |
| 45-55 MPH | 500' |



W03-4

USE OF W03-4 SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7A AND W20-4A SIGNS, USING SPACING "A".



TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

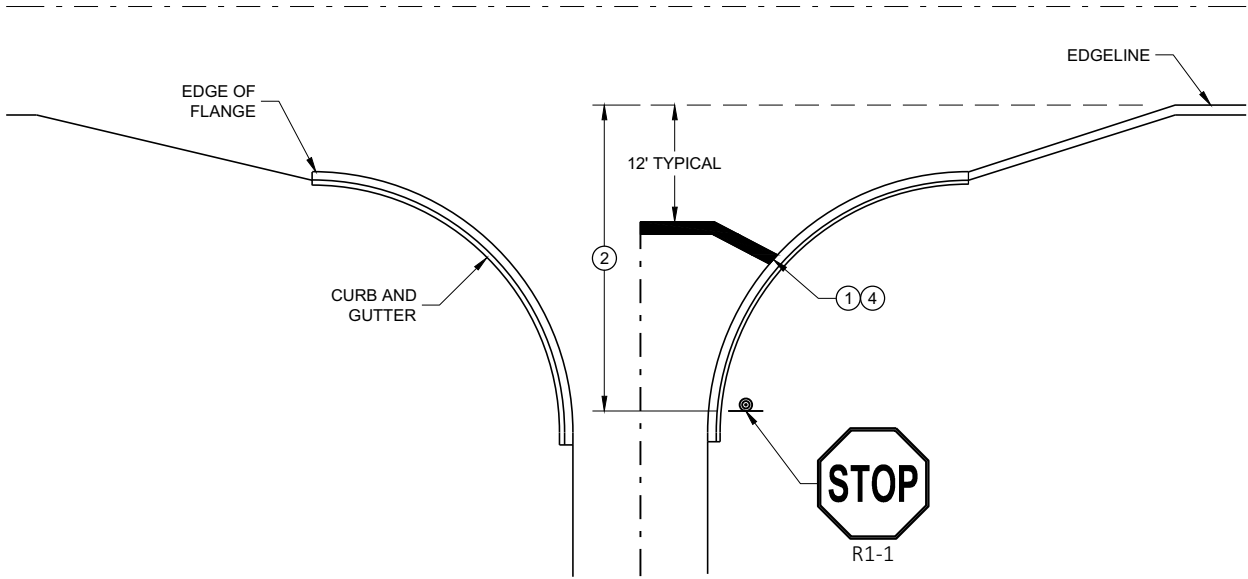
APPROVED
DATE: May 2022 /S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

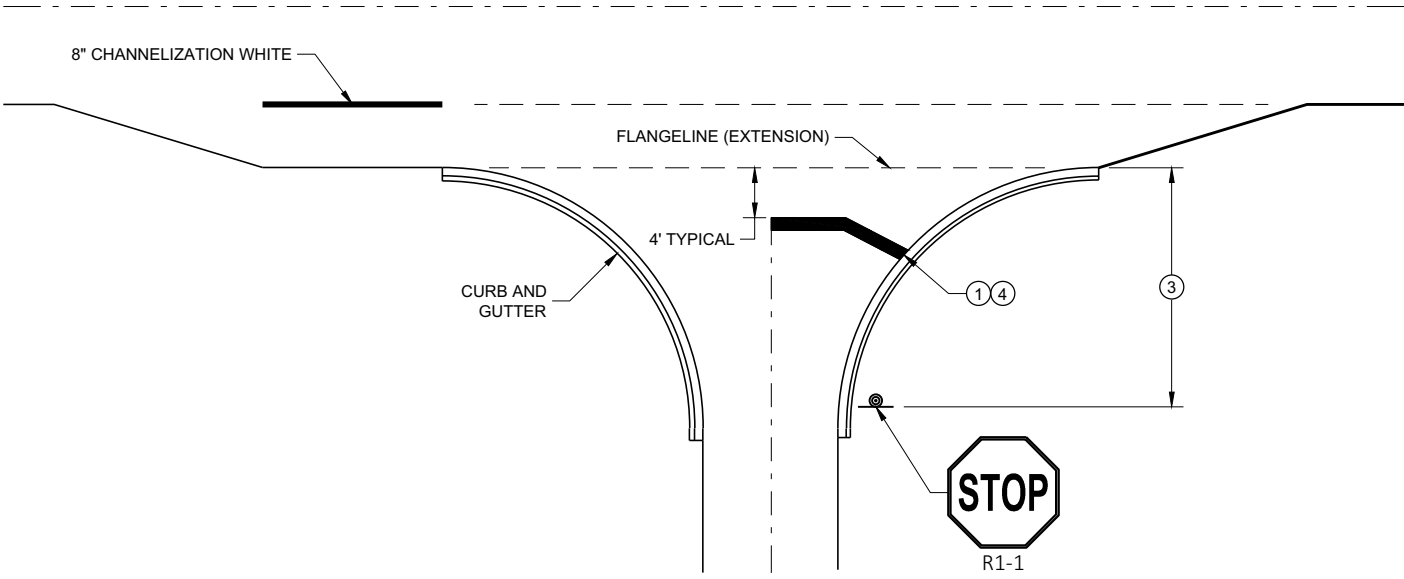
GENERAL NOTES

STOP SIGN SHALL BE PLACED A MINIMUM OF 6 FEET TO A MAXIMUM OF 50 FEET FROM THE EDGELINE LOCATION.

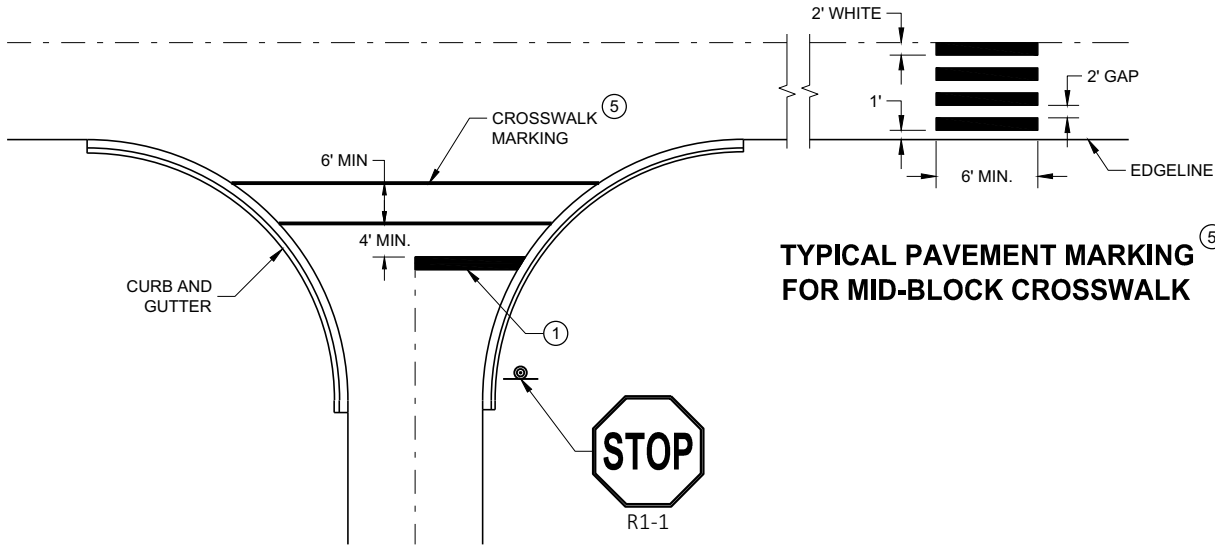
- ① 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE REGION MARKING ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
- ② NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGELINE.
- ③ NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGELINE EXTENSION.
- ④ MOVE CLOSER TO THE EDGE OF TRAVEL LINE AS NEEDED FOR VISIBILITY AND SIGHT LINES (NO CLOSER THAN 4 FEET).
- ⑤ LADDER BAR CROSSWALKS SHOULD ONLY BE USED FOR MID BLOCK CROSSINGS. USE 2 - 6" TRANSVERSE LINES INSTEAD.



TYPICAL STOP LINE PAVEMENT MARKING WITH CURB AND GUTTER

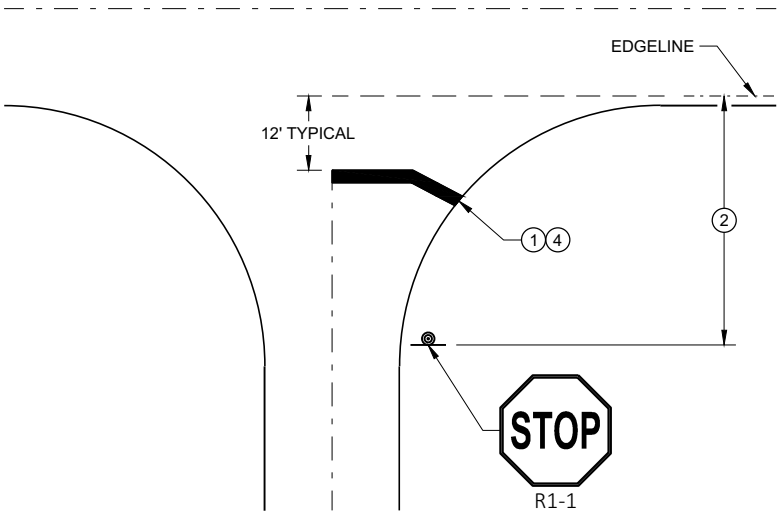


TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH RIGHT TURN LANE



TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH CROSSWALK MARKING

TYPICAL PAVEMENT MARKING FOR MID-BLOCK CROSSWALK



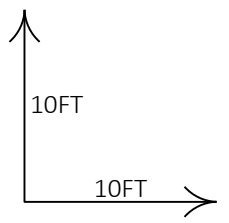
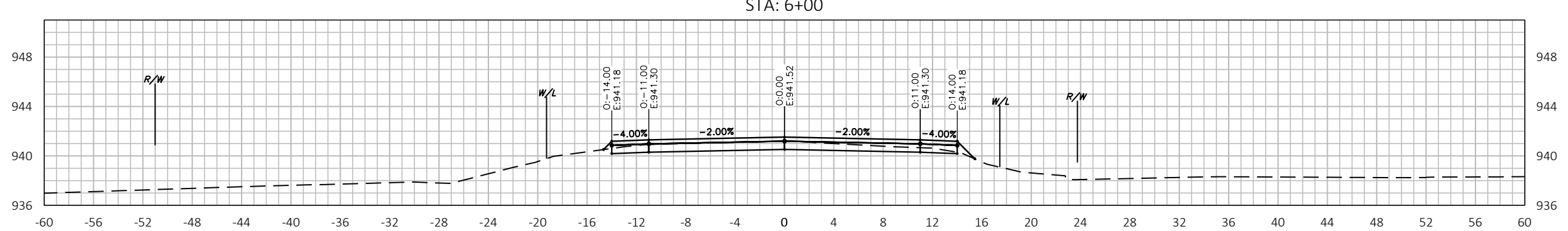
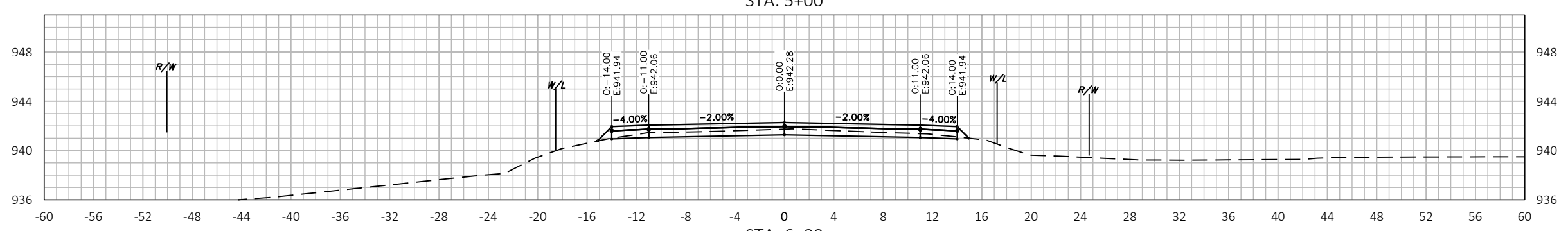
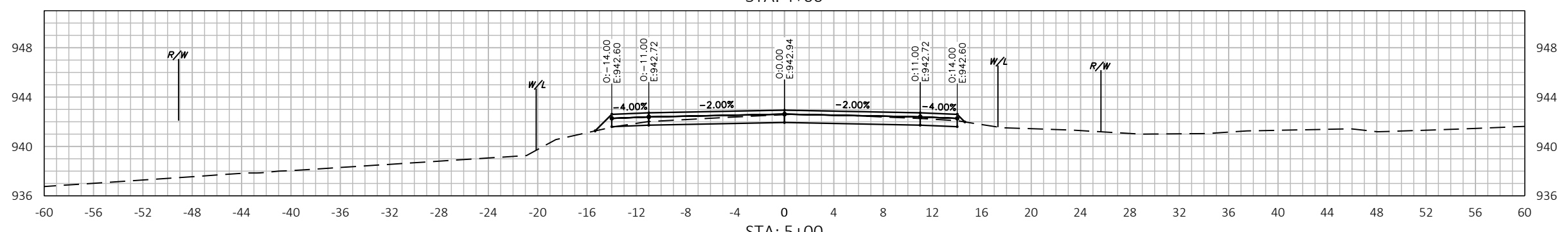
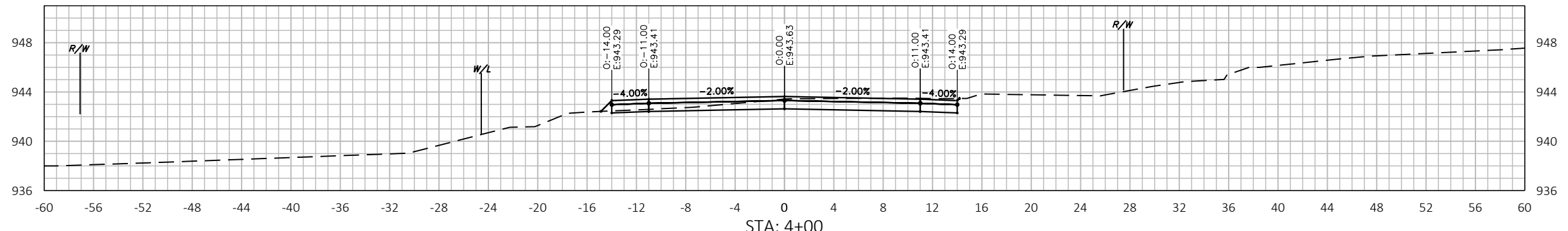
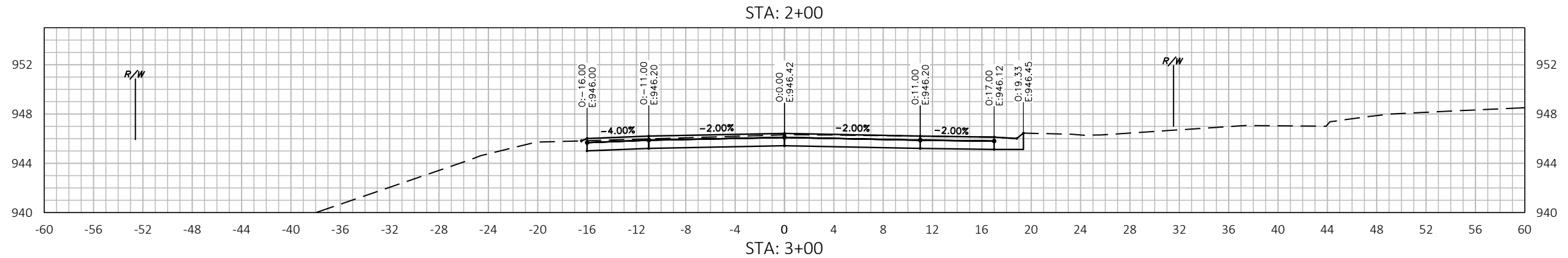
TYPICAL STOP LINE PAVEMENT MARKING WITHOUT CURB AND GUTTER

STOP LINE AND CROSSWALK PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

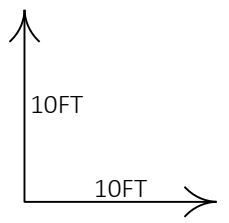
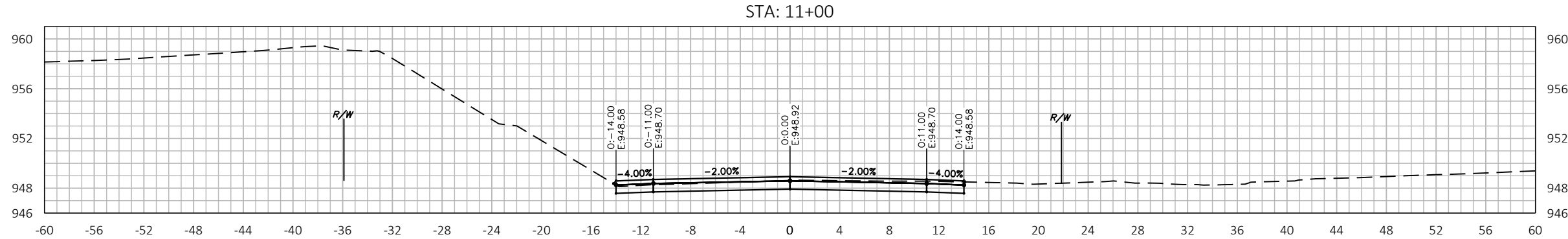
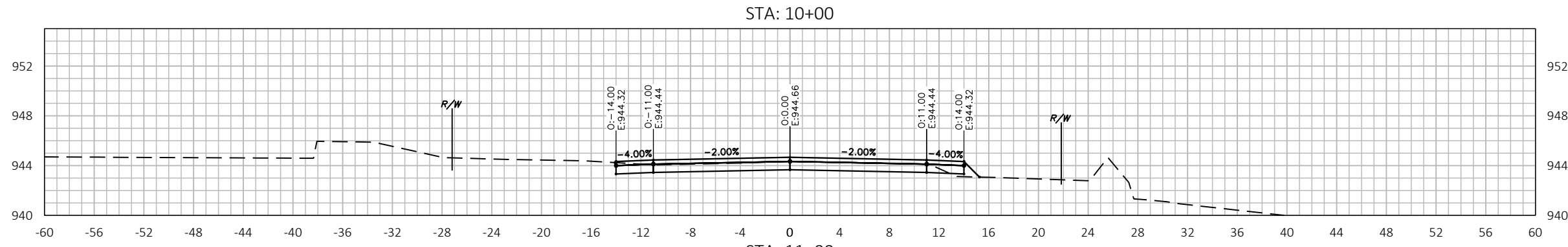
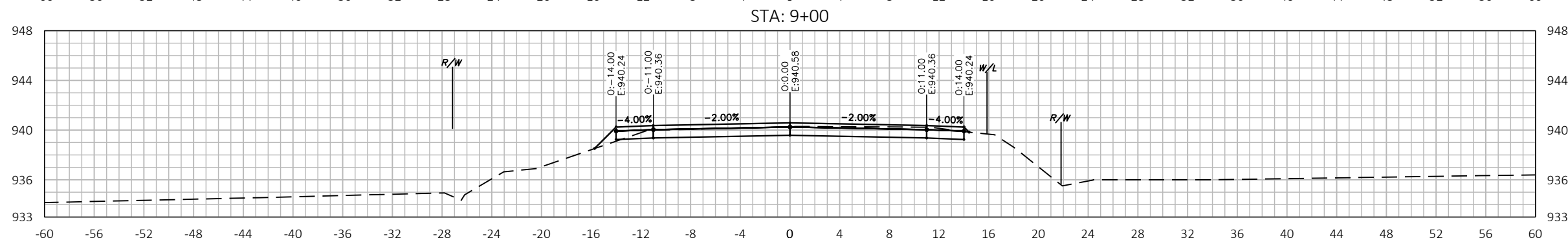
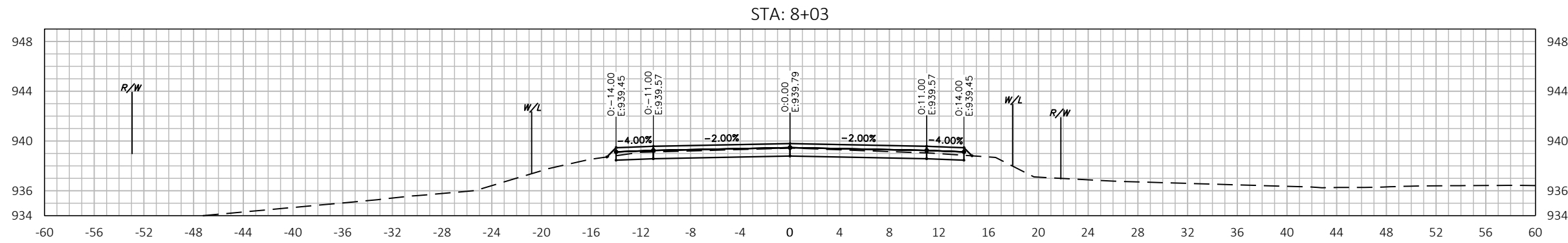
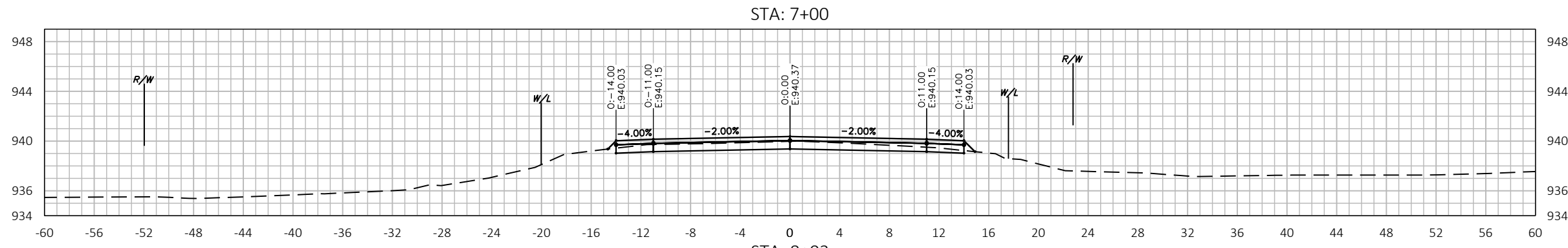
APPROVED
November 2019 /S/ Matthew Rauch
DATE STATE SIGNING AND MARKING ENGINEER

FHWA



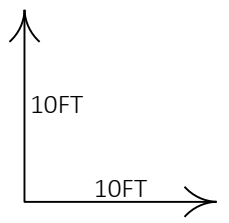
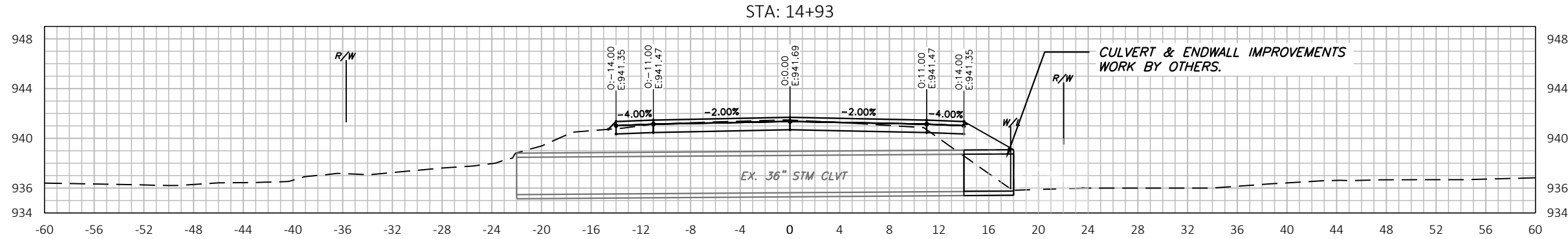
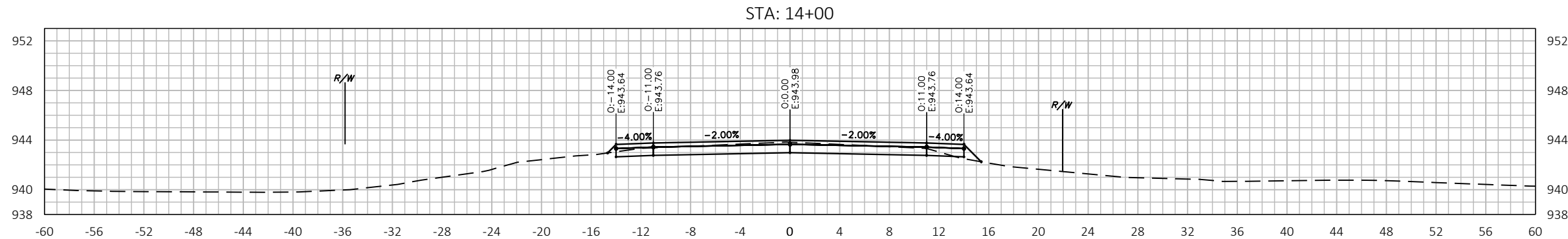
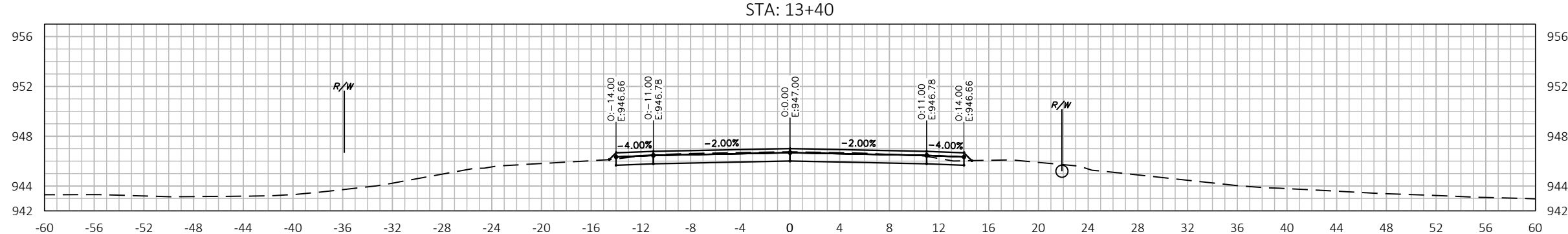
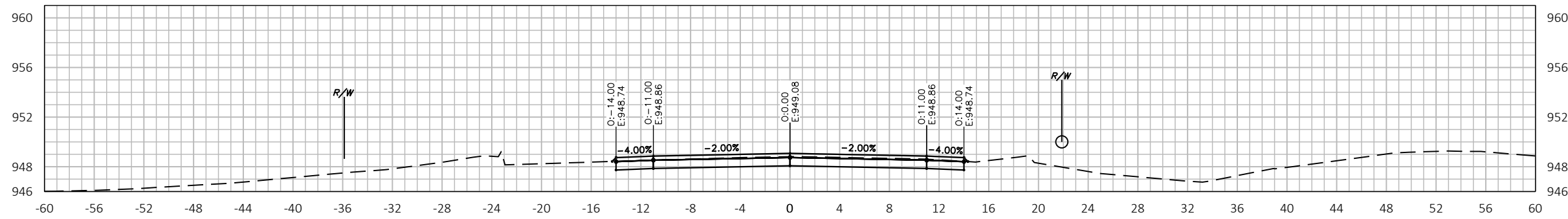
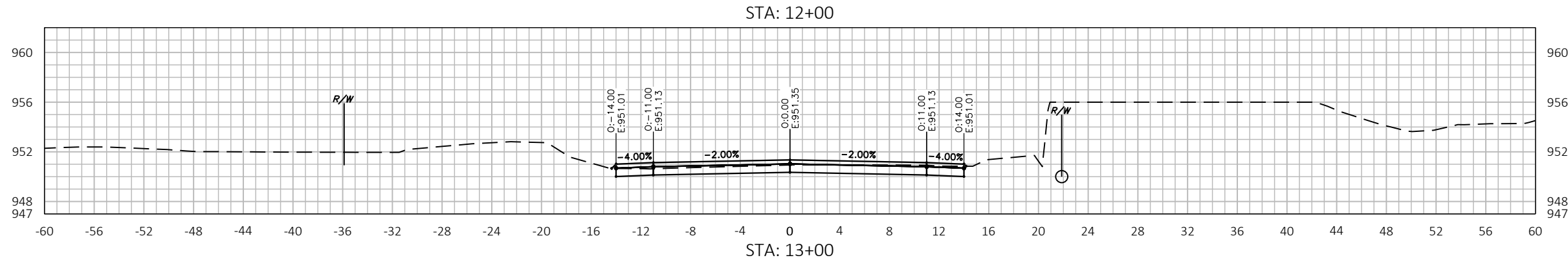
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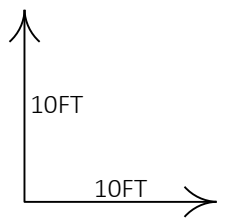
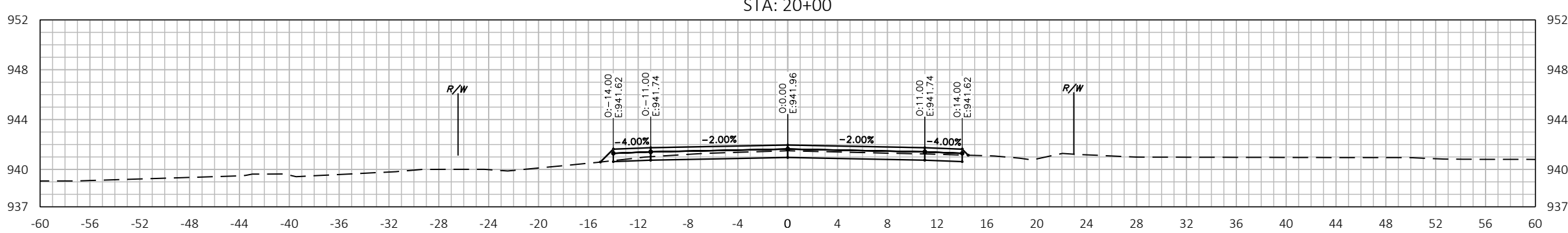
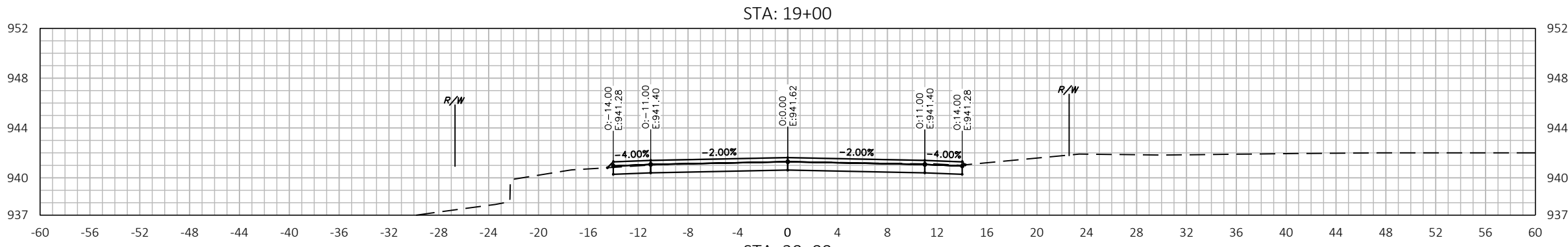
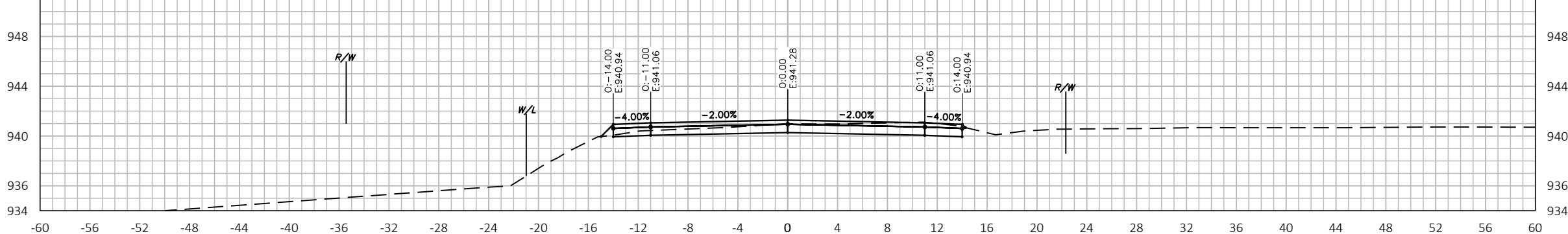
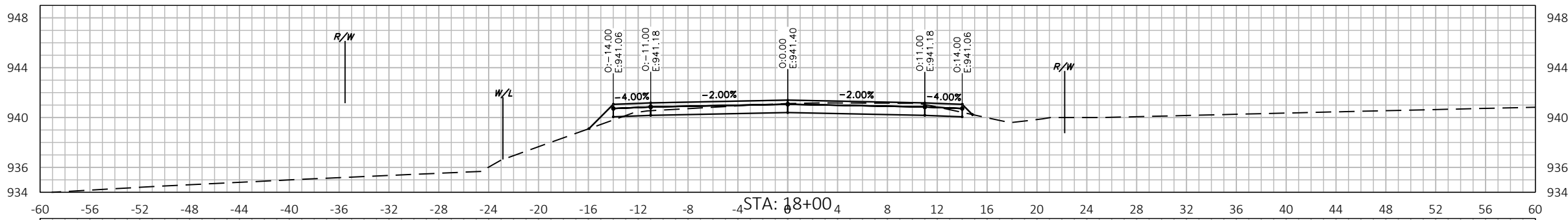
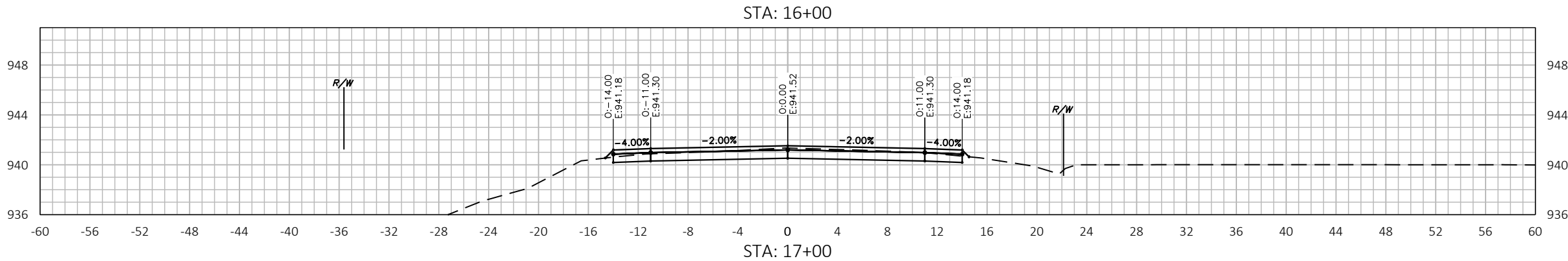
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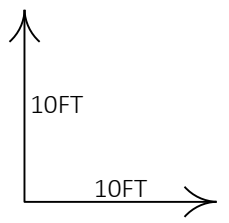
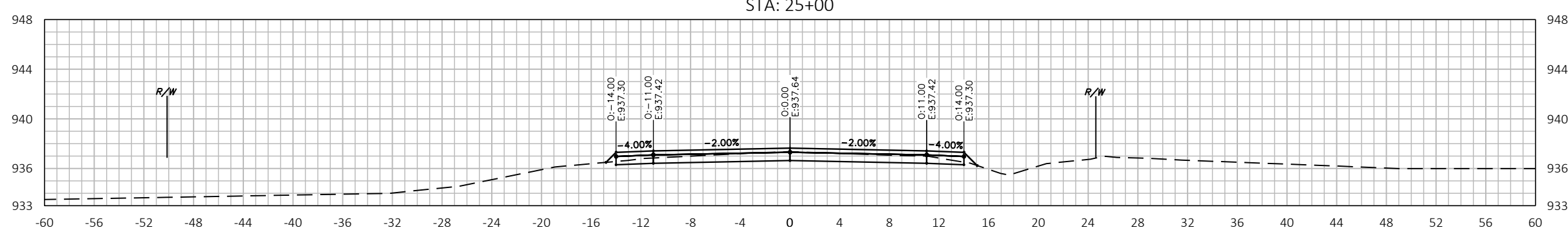
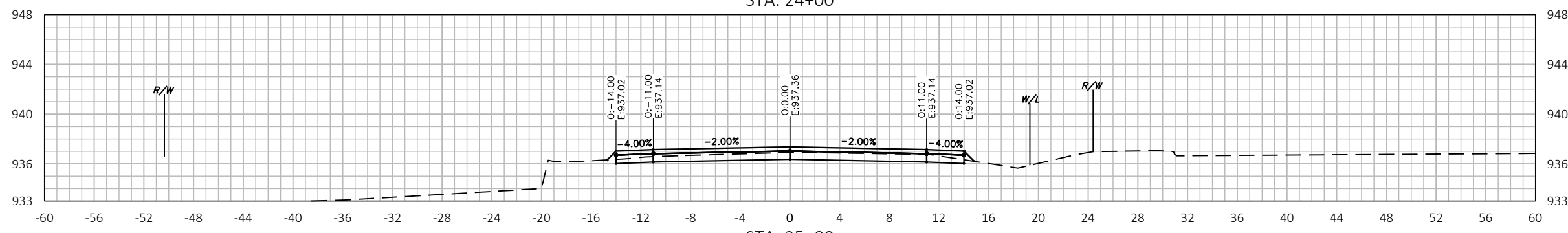
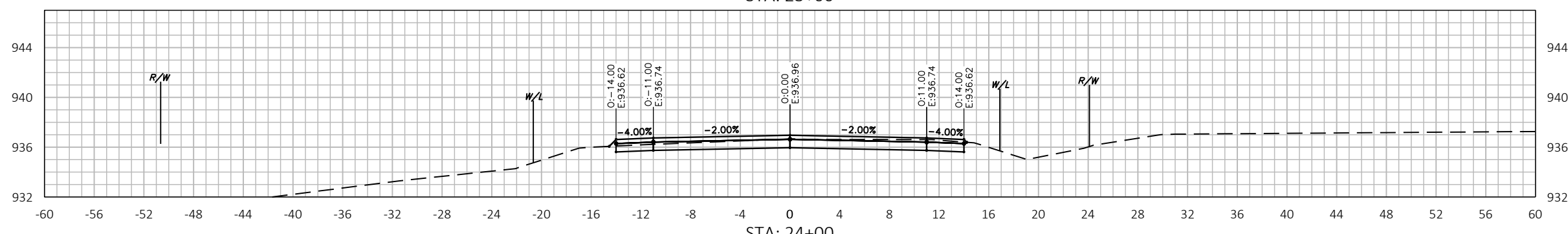
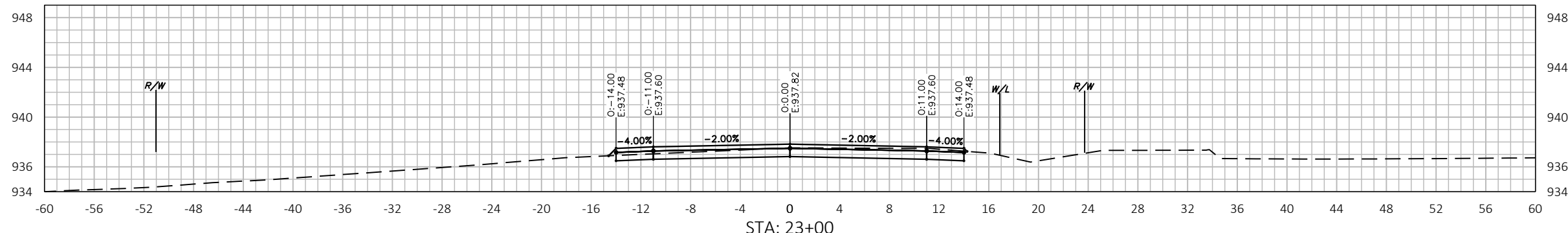
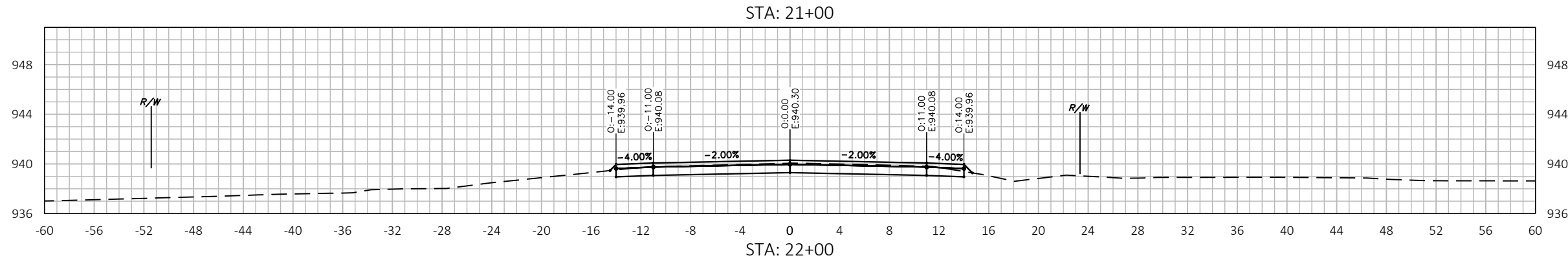
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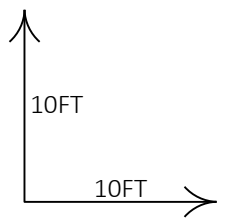
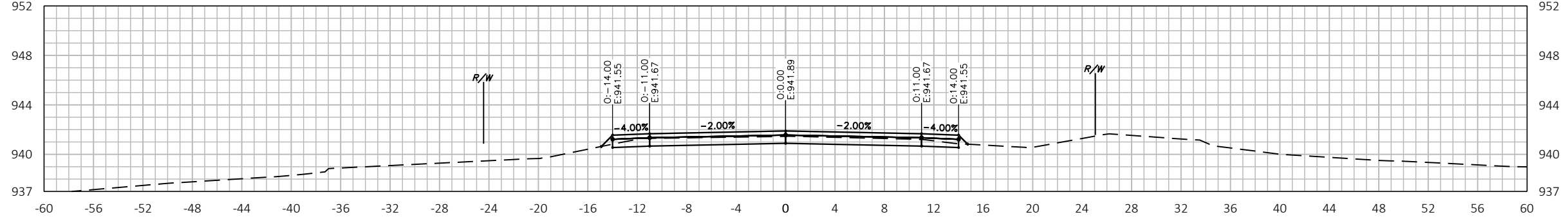
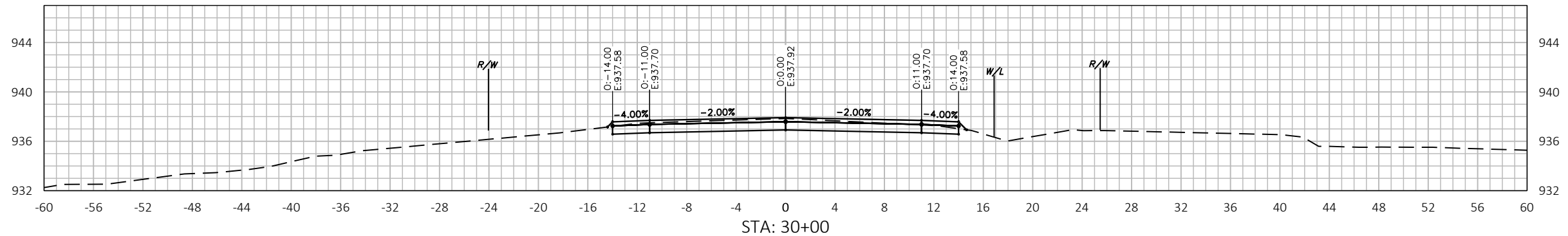
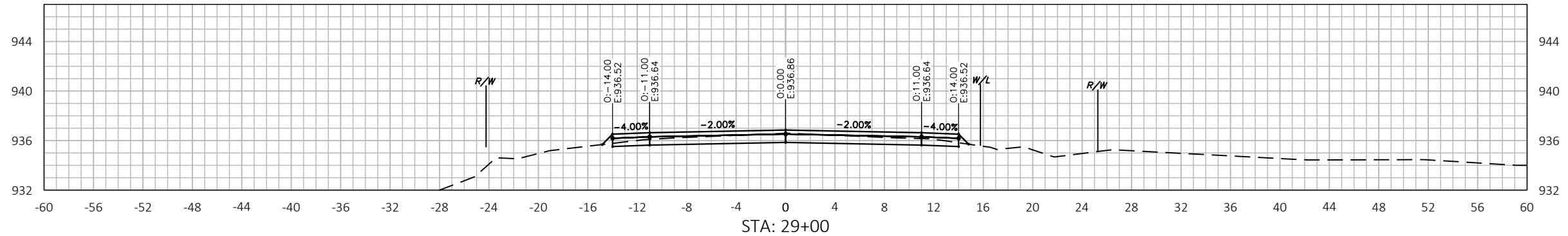
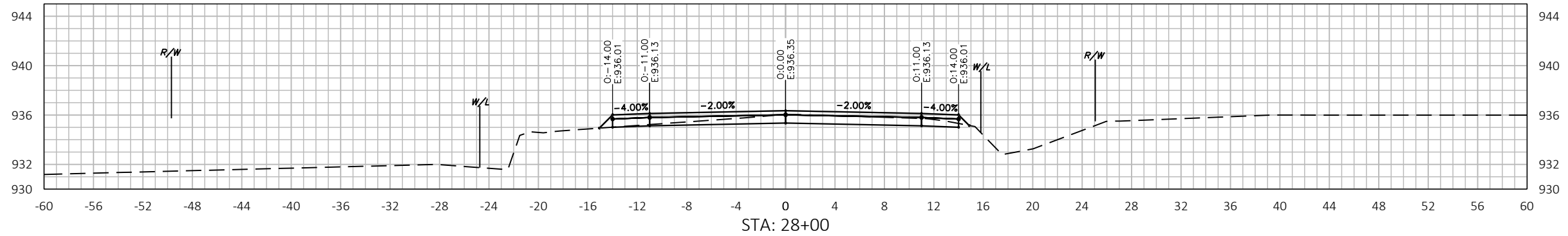
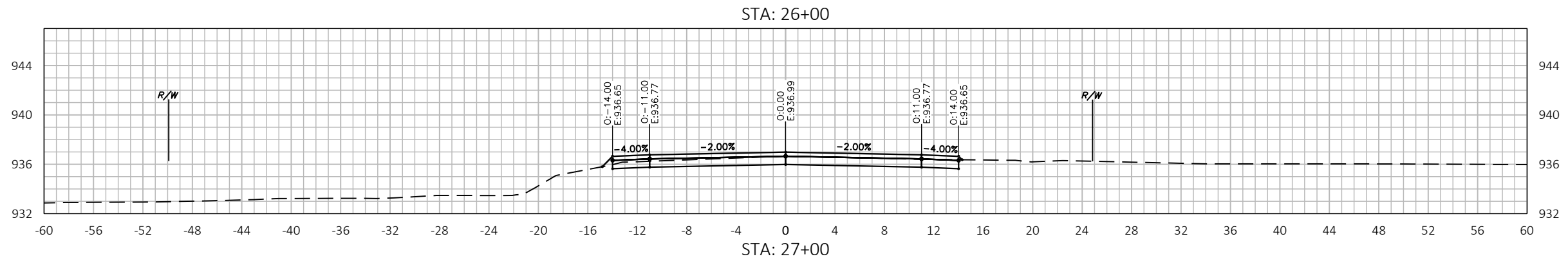
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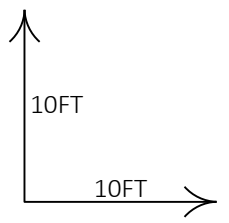
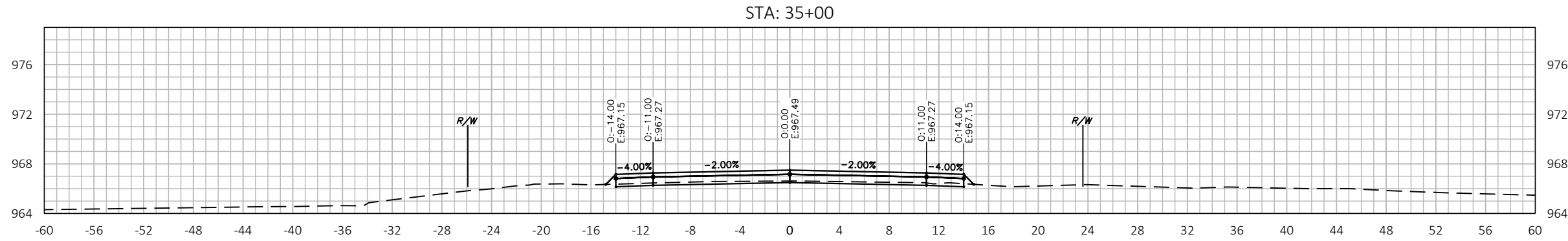
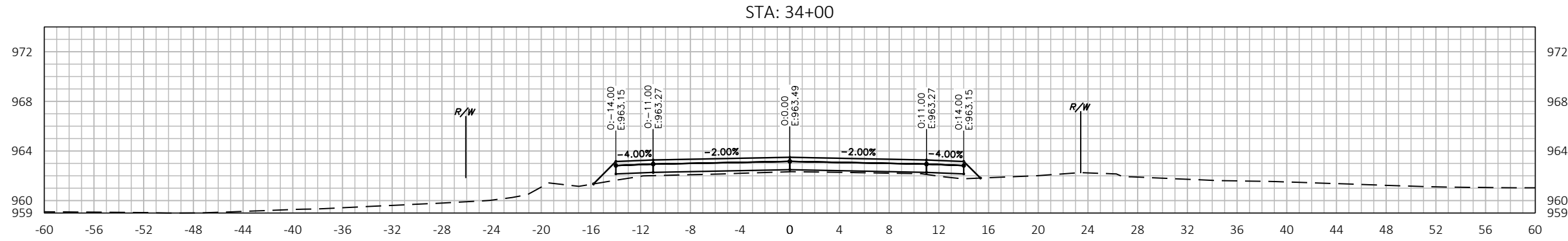
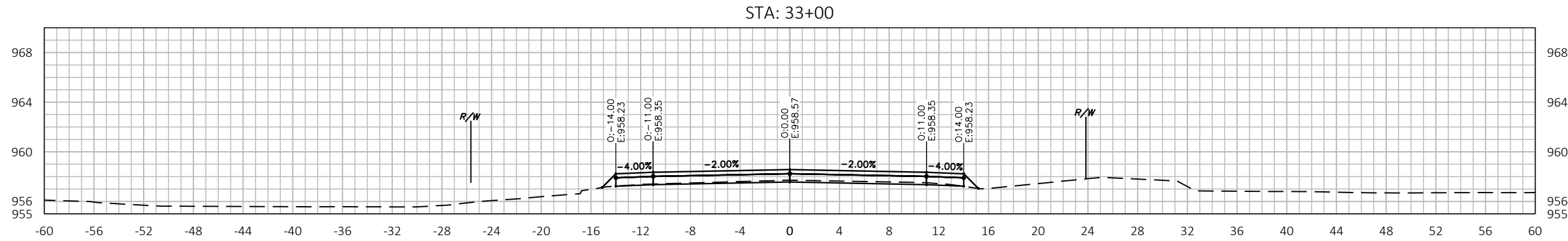
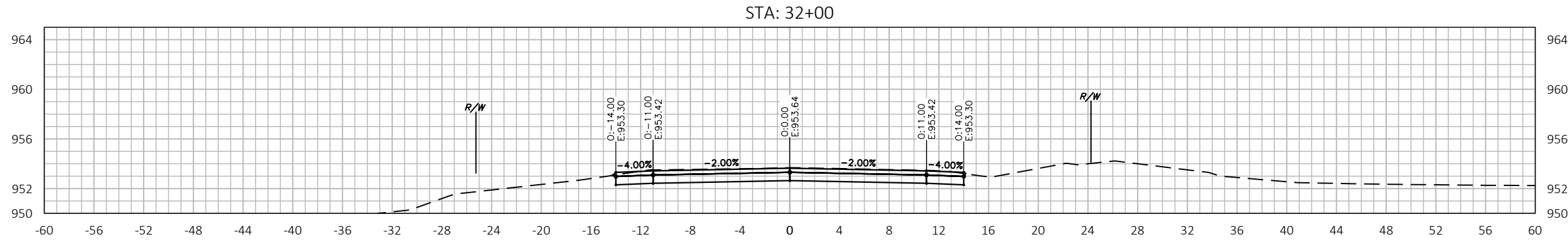
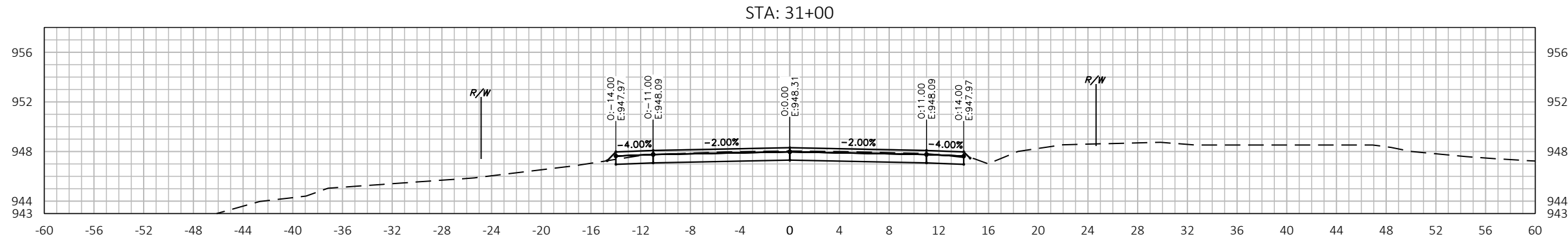
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PROJECT NO: 2707-03-74

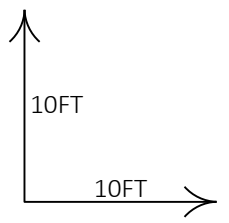
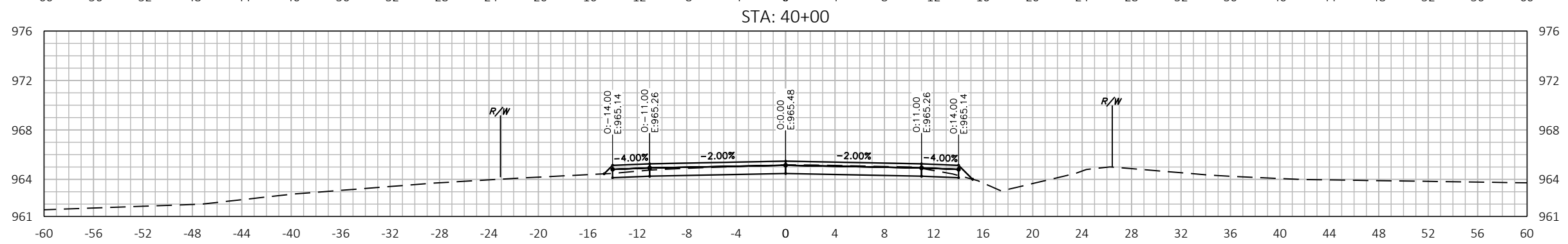
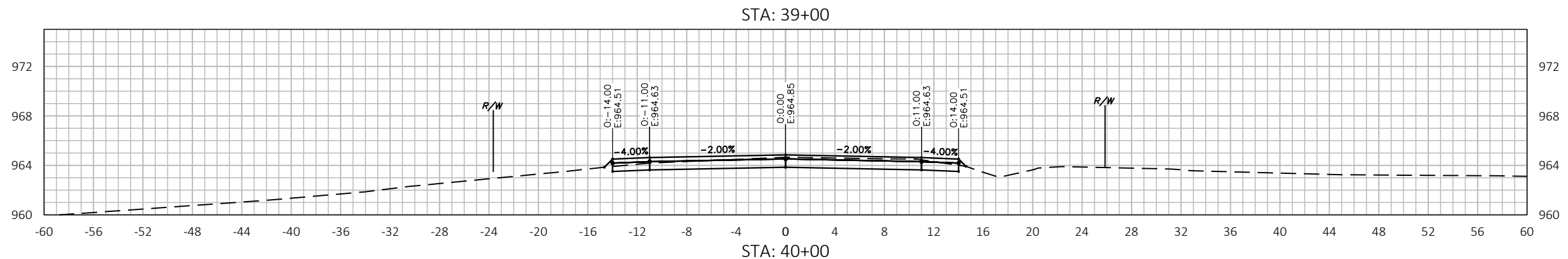
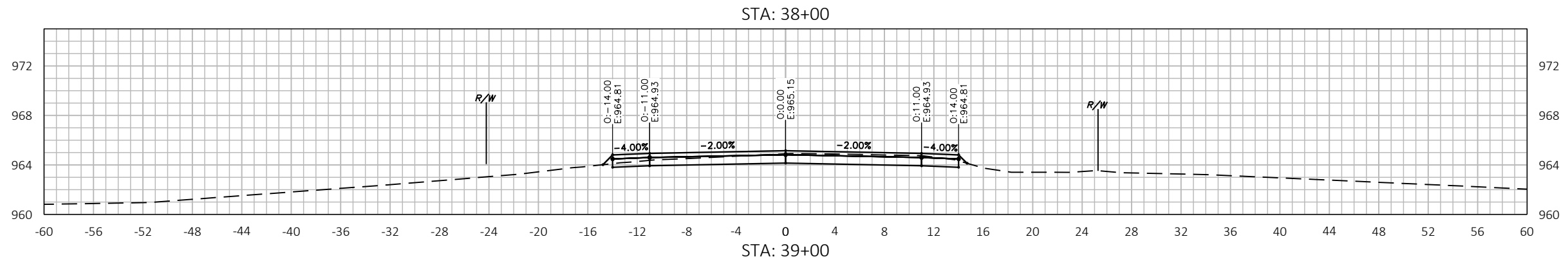
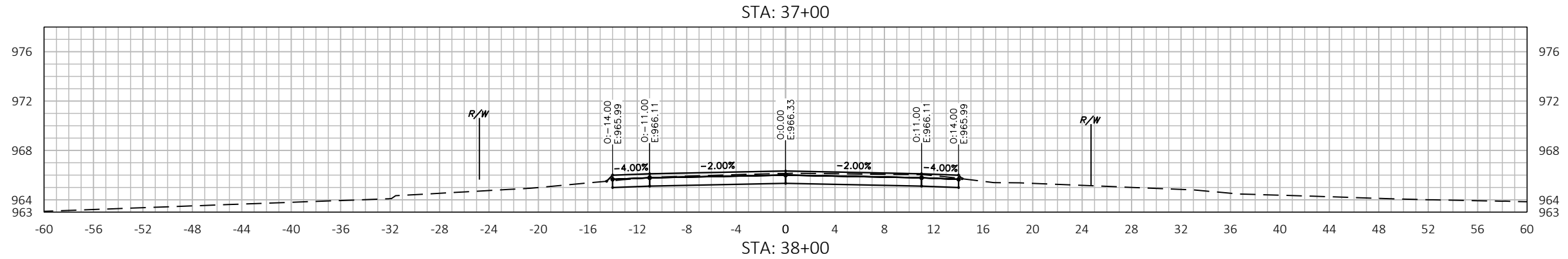
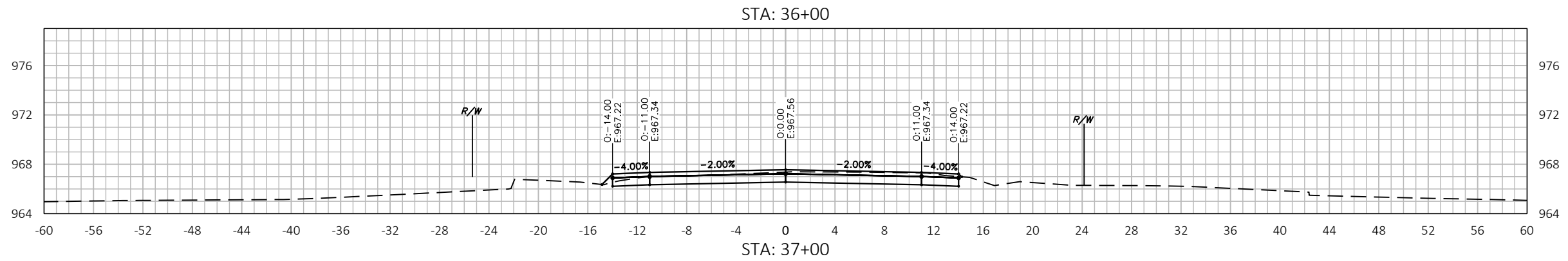
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COUNTY: WASHINGTON COUNTY

CROSS SECTIONS: RUSCO DRIVE

SHEET XS7

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PROJECT NO: 2707-03-74

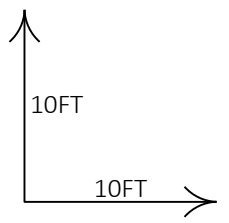
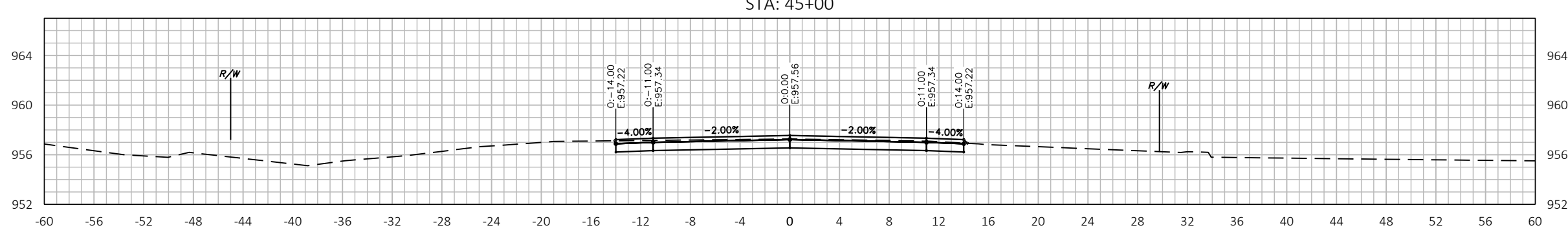
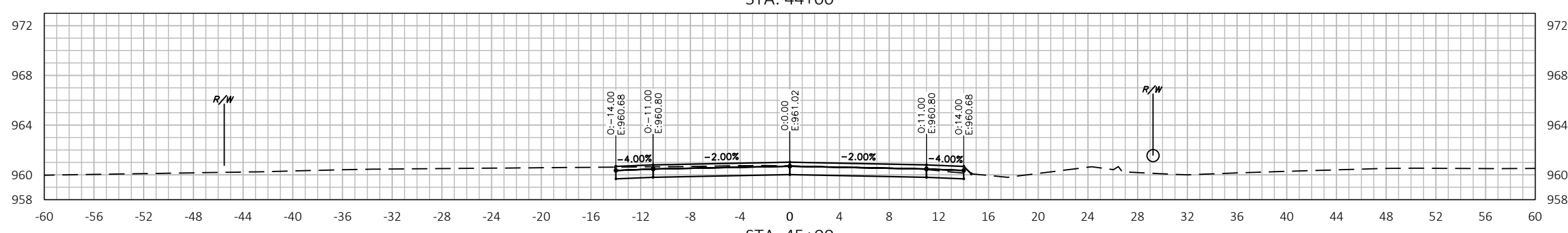
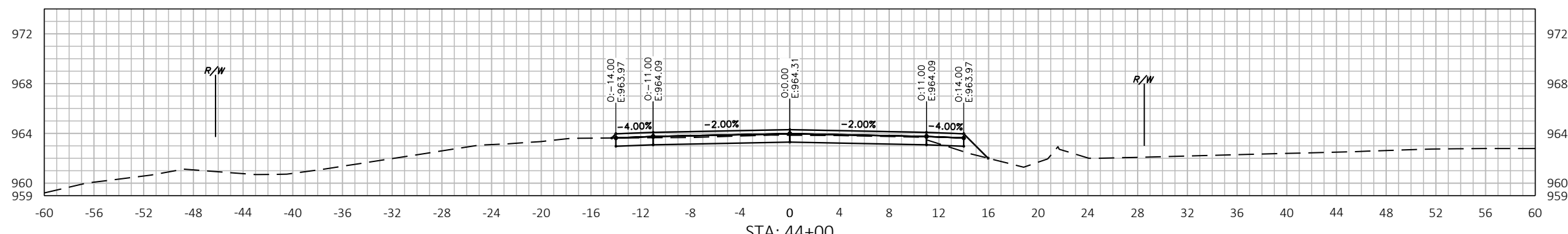
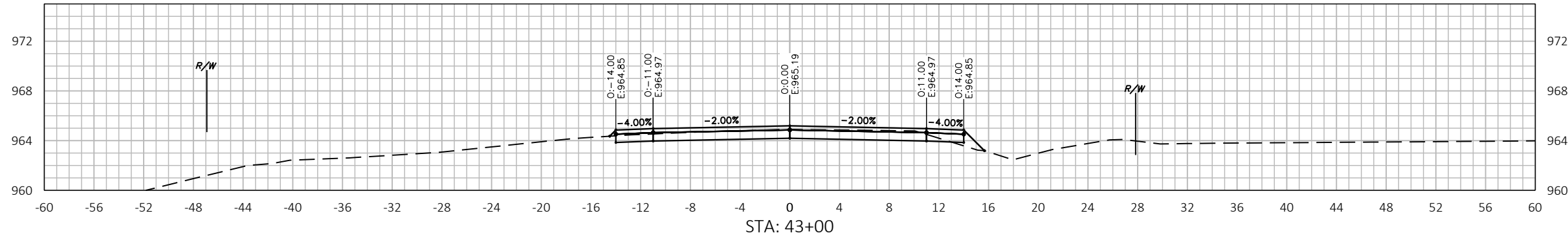
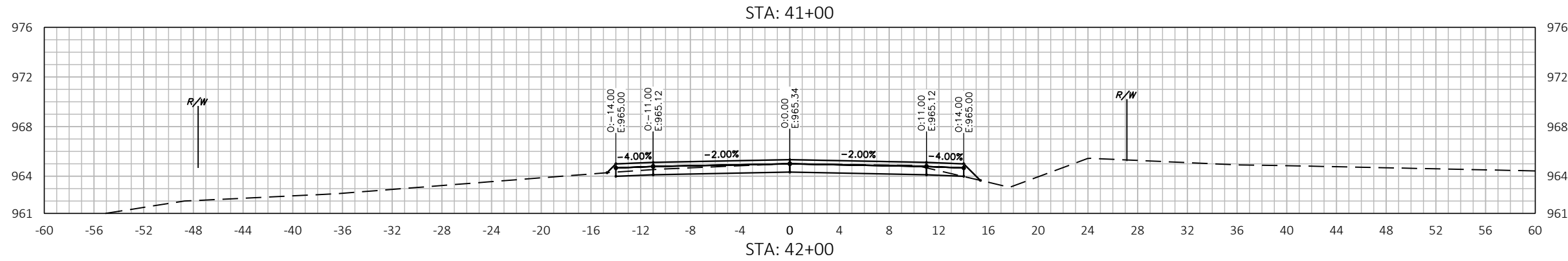
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COUNTY: WASHINGTON COUNTY

CROSS SECTIONS: RUSCO DRIVE

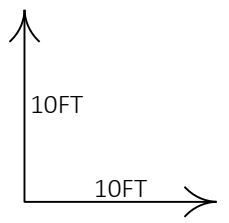
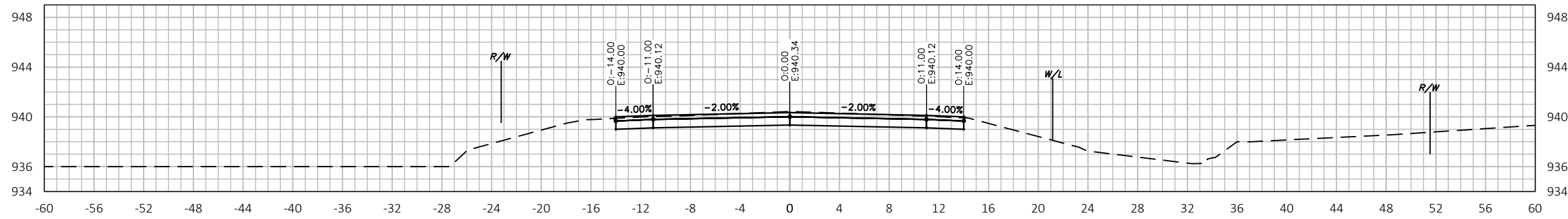
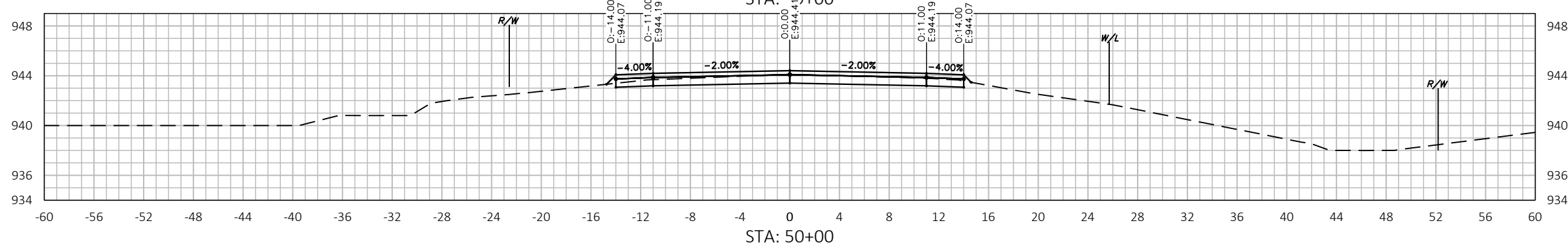
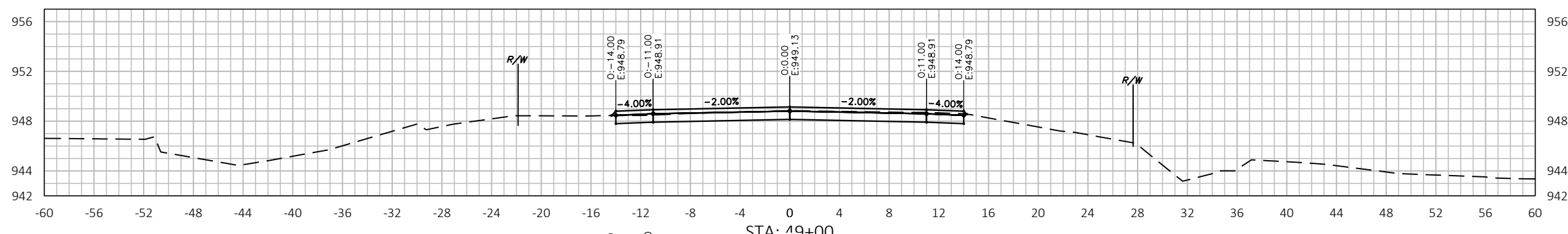
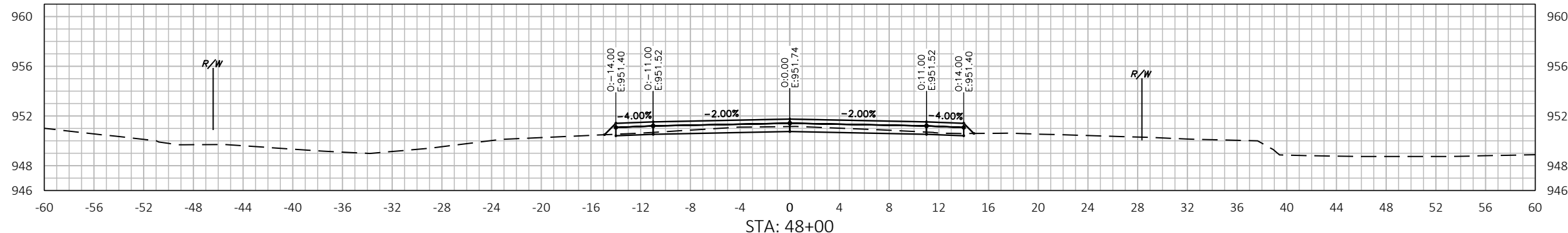
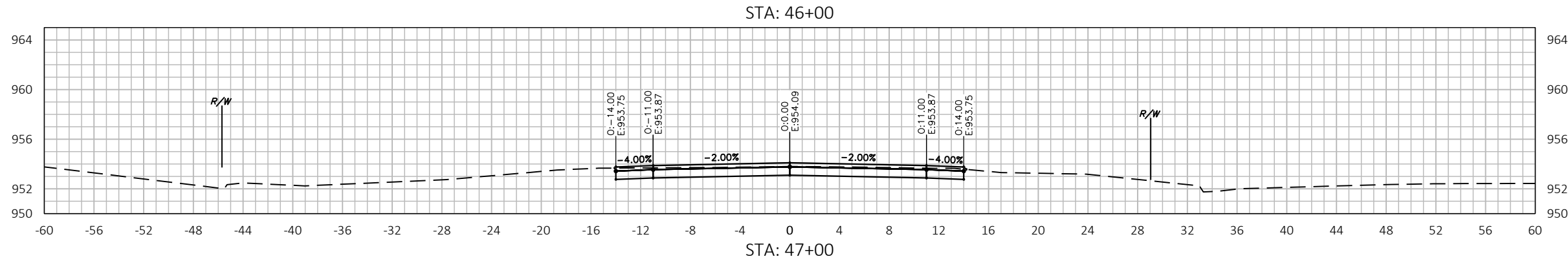
SHEET XS8

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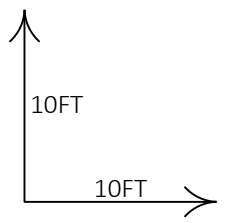
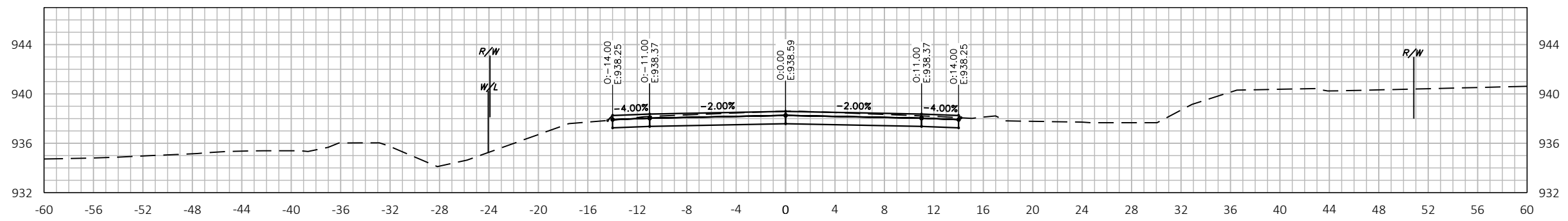
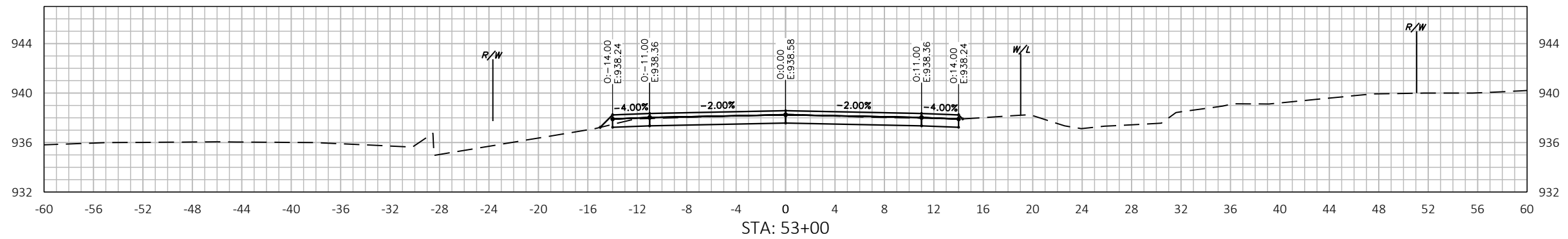
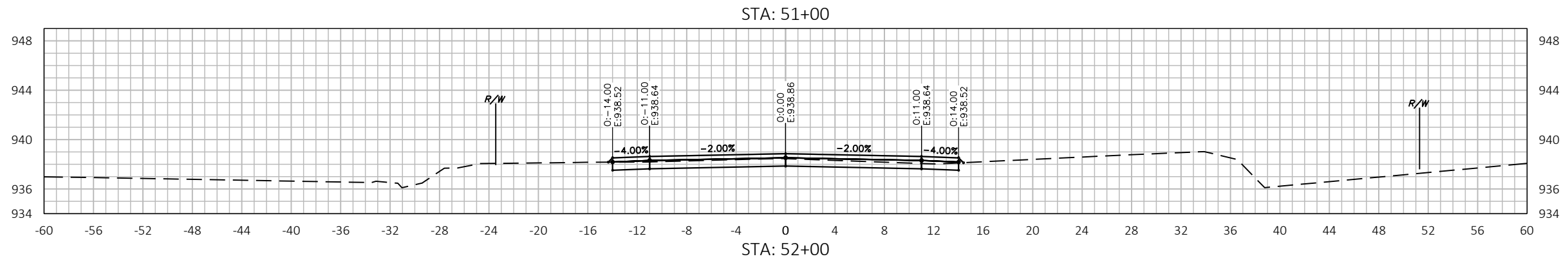
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Wisconsin Department of Transportation

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