

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
3834-00-72	WISC 2023034	1

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

JEFFERSON STREET

BRIDGE OVER FOX RIVER B-51-0001

LOCAL STREET
RACINE COUNTY

STATE PROJECT NUMBER
3834-00-72

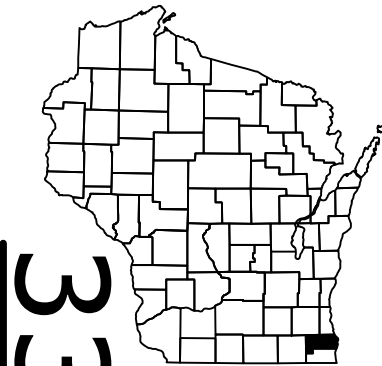
ORDER OF SHEETS

Section No. 1	Title
Section No. 2	Typical Sections and Details (Includes Erosion Control Plans)
Section No. 3	Estimate of Quantities
Section No. 3	Miscellaneous Quantities
Section No. 4	Right of Way Plat
Section No. 5	Plan and Profile
Section No. 6	Standard Detail Drawings
Section No. 7	Sign Plates
Section No. 8	Structure Plans
Section No. 9	Computer Earthwork Data
Section No. 9	Cross Sections

TOTAL SHEETS = 132

PROJECT ID: 3834-00-72

COUNTY: RACINE



DESIGN DESIGNATION

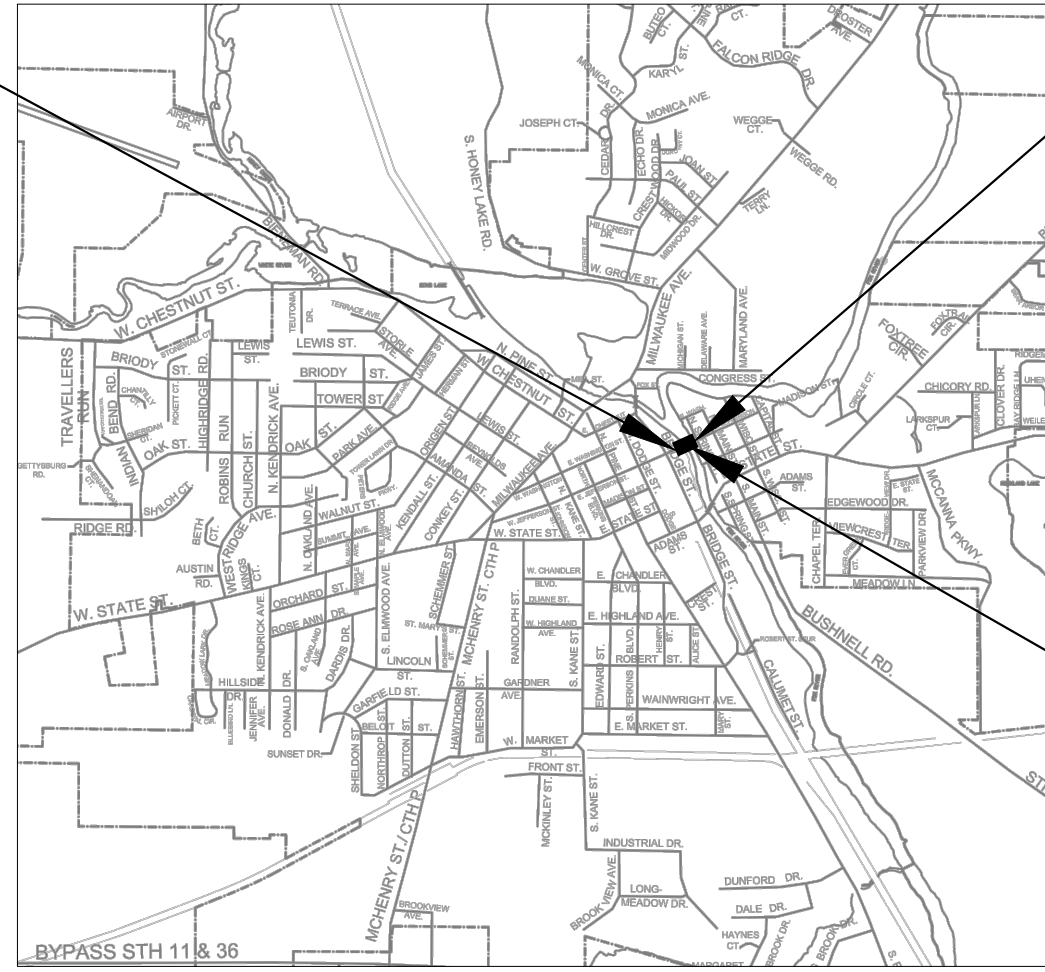
A.A.D.T. 2023	=	4,400
A.A.D.T. 2043	=	4,900
D.H.V.	=	610
D.D.	=	59/41
T.	=	5.2
DESIGN SPEED	=	30 MPH
ESALS	=	460,000

CONVENTIONAL SYMBOLS

PLAN	
CORPORATE LIMITS	
PROPERTY LINE	
LOT LINE	
LIMITED HIGHWAY EASEMENT	
EXISTING RIGHT OF WAY	
PROPOSED OR NEW R/W LINE	
SLOPE INTERCEPT	
REFERENCE LINE	
EXISTING CULVERT	
PROPOSED CULVERT (Box or Pipe)	
COMBUSTIBLE FLUIDS	
MARSH AREA	
WOODED OR SHRUB AREA	

PROFILE

GRADE LINE	
ORIGINAL GROUND	
MARSH OR ROCK PROFILE (To be noted as such)	
SPECIAL DITCH	
GRADE ELEVATION	
CULVERT (Profile View)	
UTILITIES	
ELECTRIC	
OVERHEAD UTILITY	
FIBER OPTIC	
GAS	
SANITARY SEWER	
STORM SEWER	
TELEPHONE	
WATER	
UTILITY PEDESTAL	
POWER POLE	
TELEPHONE POLE	



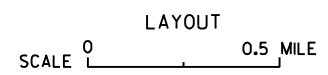
BEGIN PROJECT
STA 11+15.00
Y=168,901.586
X=508,372.351

END PROJECT
STA 14+95.00
Y=169,060.526
X=508,717.485

T-3-N

STRUCTURE B-51-0154
STA 11+93 - STA 13+97

T-2-N



TOTAL NET LENGTH OF CENTERLINE = 0.072

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COUNTY COORDINATES, RACINE COUNTY, NAD83 (2011) IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ELEVATIONS SHOWN ON THE PLAN ARE REFERENCED TO NAVD 88 (2012)

ACCEPTED FOR CITY OF BURLINGTON
DATE: 7/24/22
[Signature]
Director of Public Works
(Title of Official)

ORIGINAL PLANS PREPARED BY
CORRE ENGINEERING
20900 SWENSON DR
SUITE 800
WAUKESHA, WI 53186
(608)828-1011
www.correinc.com

WISCONSIN PROFESSIONAL ENGINEER
JESSICA D. LEWIS
E-37589
Lake Geneva, WI
DATE: 7/28/22
[Signature]
(Signature)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY
Surveyor _____ CORRE, INC.
Designer _____ CORRE, INC.
Project Manager _____ MICHAEL BAIRD
Regional Examiner _____ WISDOT
Regional Supervisor _____ JEFFREY BOHEN

APPROVED FOR THE DEPARTMENT
DATE: 7/28/2022
[Signature]
(Signature)

GENERAL NOTES

1. THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN. COORDINATE CONSTRUCTION ACTIVITIES WITH A CALL TO DIGGERS HOTLINE AND/OR A DIRECT CALL TO THE UTILITIES THAT HAVE FACILITIES IN THE AREA. NOT ALL UTILITIES ARE MEMBERS OF DIGGERS HOTLINE.
2. CURB AND GUTTER GRADES ARE GIVEN TO THE FLANGE OF CURB AND GUTTER OR THEORETICAL FLANGE POINT OF INTEGRAL CURB AND GUTTER. CURB AND GUTTER RADII ARE MEASURED TO THE FLANGE OF CURB AND GUTTER OR THEORETICAL FLANGE POINT OF INTEGRAL CURB AND GUTTER.
3. FOR INLET AND CATCH BASIN STRUCTURES LOCATED IN THE CURB OR IN FRONT OF BARRIER, STATION, OFFSET, AND ELEVATIONS ARE GIVEN TO THE FLOW LINE. MH STATION, OFFSET, AND ELEVATIONS ARE GIVEN TO THE CENTER OF STRUCTURE. SEE CONSTRUCTION DETAILS.
4. CONTACT THE PROJECT ENGINEER, THE COUNTY SURVEYOR, AND SEWRPC AT LEAST TWO WEEKS BEFORE WORKING NEAR ANY SECTION CORNER MONUMENT.
5. VERIFY EXISTING PAVEMENT ELEVATIONS AT ALL TIE-INS TO EXISTING PAVEMENT PRIOR TO CONSTRUCTION. NOTIFY ENGINEER IF A DISCREPANCY IS FOUND BETWEEN PROPOSED PLAN ELEVATIONS AND EXISTING PAVEMENT ELEVATIONS.
6. CONSTRUCT PAVEMENT CONSISTENT WITH THE PLAN TYPICAL SECTIONS. LOCATE LONGITUDINAL JOINTS IN ASPHALT PAVEMENT OUTSIDE OF DRIVING, TURNING, BIKE, OR PARKING LANE UNLESS DIRECTED OTHERWISE BY THE ENG PREVENT HMA LONGITUDINAL JOINTS FROM BEING LOCATED WITHIN A DRIVING, TURNING, BIKE, OR PARKING LANE. THE CONTRACTOR'S PAVING OPERATIONS SHALL BE CONSISTENT WITH THE PLAN TYPICAL SECTIONS AND CONSTRUCTED TO PREVENT HMA LONGITUDINAL JOINTS FROM BEING LOCATED WITHIN A DRIVING, TURNING, BIKE OR PARKING LANE.
7. SAWCUT EXISTING ASPHALT AND CONCRETE PAVEMENT AT THE MATCHLINE AS INDICATED ON THE PLAN OR AS DIRECTED BY THE ENGINEER.
8. TINE CONCRETE PAVEMENT LONGITUDINALLY UNLESS OTHERWISE IDENTIFIED IN THE PLAN OR DIRECTED BY THE ENGINEER.
9. CONSTRUCT INSIDE EDGE OF SIDEWALK 1/2 INCH HIGHER THAN TOP OF CURB WHEN THEY ARE ADJACENT TO EACH OTHER.
10. RESHAPE, RESTORE AND FINISH ALL PREVIOUSLY GRASSED AREAS DISTURBED BY OPERATIONS OUTSIDE OF THE NORMAL CONSTRUCTION LIMITS AT NO EXPENSE TO THE DEPARTMENT
11. PLACE TOPSOIL 1 INCH BELOW THE TOP OF ADJACENT CONCRETE CURBS OR SIDEWALKS IN SOD AREAS.
12. DISTURBED AREAS WITHIN THE RIGHT OF WAY ARE TO BE TOPSOILED (SALVAGED), FERTILIZED, AND SODDED AS DIRECTED BY THE ENGINEER.
13. WHEN DEFINING THE PAVEMENT STRUCTURE, THE BOTTOM OF THE BASE AGGREGATE DENSE IS CONSIDERED THE SUBGRADE LINE.
14. BACKFILL ALL OPENINGS AND HOLES LOCATED WITHIN THE ROADWAY RESULTING FROM REMOVALS OR ABANDONMENTS WITH GRANULAR BACKFILL UNLESS THE PLANS PROVIDE FOR ALTERNATE BACKFILL.
15. DO NOT REMOVE ANY TREES OR SHRUBS WITHOUT APPROVAL OF THE ENGINEER.
16. ALL SIGN LOCATIONS SHALL BE REVIEWED BY THE ENGINEER PRIOR TO INSTALLATION.
17. EXISTING DRIVEWAYS AND FIELD ENTRANCES SHALL BE RESTORED IN KIND AS DIRECTED BY THE ENGINEER IN THE FIELD AND AT THE LOCATION DETERMINED BY THE ENGINEER.
18. THE EXACT LOCATION OF PRIVATE ENTRANCES IS TO BE DETERMINED IN THE FIELD BY THE ENGINEER
19. TOPSOIL SHALL BE REPLACED WITH 6-INCH TYPICAL DEPTH.

ORDER OF SECTION 2 DETAIL SHEETS:

- PROJECT OVERVIEW
- TYPICAL SECTIONS
- CONSTRUCTION DETAILS
- CURB RAMP DETAILS
- EROSION CONTROL
- PERMANENT SIGNING & PAVEMENT MARKING
- LIGHTING
- DETOUR PLAN
- ALIGNMENT

STANDARD ABBREVIATIONS

AP	ACCESS POINT	INTERS	INTERSECTION
AC	ACRE	INV	INVERT
AGG	AGGREGATE	JT	JOINT
ASPH	ASPHALTIC	LT	LEFT
BL	BASELINE	LF	LINEAR FOOT
BM	BENCH MARK	MH	MANHOLE
CB	CATCH BASIN	MP	MARKER POST
CL	CENTER LINE	MB	MESSAGE BOARD
CONC	CONCRETE	NOM	NOMINAL
CO	COUNTY	NB	NORTHBOUND
CABC	CRUSHED AGGREGATE BASE COURSE	PAVT	PAVEMENT
CY	CUBIC YARD	PERM	PERMANENT
CULV	CULVERT	PU	PIPE UNDERDRAIN
CP	CULVERT PIPE	PCC	PORTLAND CEMENT CONCRETE
C&G	CURB AND GUTTER	PE	PRIVATE ENTRANCE
DIA	DIAMETER	PROJ	PROJECT
DWY	DRIVEWAY	PL	PROPERTY LINE
EB	EASTBOUND	RL	REFERENCE LINE
ELEV	ELEVATION	RT	RIGHT
EW	ENDWALL	R/W	RIGHT OF WAY
ENT	ENTRANCE	RDWY	ROADWAY
EXC	EXCAVATION	SHLDR	SHOULDER
FP	FENCE POST	SB	SOUTHBOUND
FERT	FERTILIZE	SS	STORM SEWER
F	FILL	TEL	TELEPHONE
FG	FINISHED GRADE	TEMP	TEMPORARY
FL	FLOW LINE	TER	TERRACE
FO	FIBER OPTIC	TV	TELEVISION
FT	FOOT	UG	UNDERGROUND
HYD	HYDRANT	VOL	VOLUME
INL	INLET	W	WATER
		WB	WESTBOUND

DNR LIASION

WISCONSIN DEPARTMENT OF NATURAL RESOURCES
 BENTON STELZEL
 141 NW BARSTOW, ROOM 180
 WAUKESHA, WI 53188
 PHONE: (262) 623-0194
 EMAIL: BENTON.STELZEL@WISCONSIN.GOV

DESIGN CONTACT

CORRE, INC.
 JESSICA LEWIS, PE
 20900 SWENSON DRIVE
 SUITE 800
 WAUKESHA, WI 53186
 PHONE: (262) 393-1508
 EMAIL: JLEWIS@CORREINC.COM

SOUTHEASTERN WISCONSIN REGIONAL PLANNING COMMISSION

ROB MERRY
 W239 N1812 ROCKWOOD DRIVE, P.O. BOX 1607
 WAUKESHA, WI 53187-1607
 PHONE: (262) 953-4289
 CELL: (920) 912-1036
 E-MAIL: RMERRY@SEWRPC.ORG

ARCHAEOLOGIST

BRIAN NICHOLLS
 UW-MILWAUKEE
 E-MAIL: NICHOLLS@UWM.EDU

UTILITY CONTACTS

COMMUNICATION LINE

AT&T WISCONSIN
 NATHAN GIBERT
 411 7TH STREET
 RACINE, WI 53403
 PHONE: (262) 720-8235
 EMAIL: NG952W@ATT.COM

WATER

CITY OF BURLINGTON
 PETER RIGGS
 2200 S PINE STREET
 BURLINGTON, WI 53105
 PHONE: (262) 342-1182
 EMAIL: PRIGGS@BURLINGTON-WI.GOV

GAS

WE ENERGIES - GAS
 SCOTT HOLSTEIN
 700 S KANE STREET
 BURLINGTON, WI 53105
 PHONE: (262) 763-1084
 EMAIL: We-Utility_Relocations@we-energies.com

COMMUNICATION LINE

CHARTER COMMUNICATIONS
 STEVE STORM
 1320 N DR MARTIN LUTHER KING DR
 MILWAUKEE, WI 53212
 PHONE: (414) 908-1339
 EMAIL: WIS.ENGINEERING@CHARTER.COM

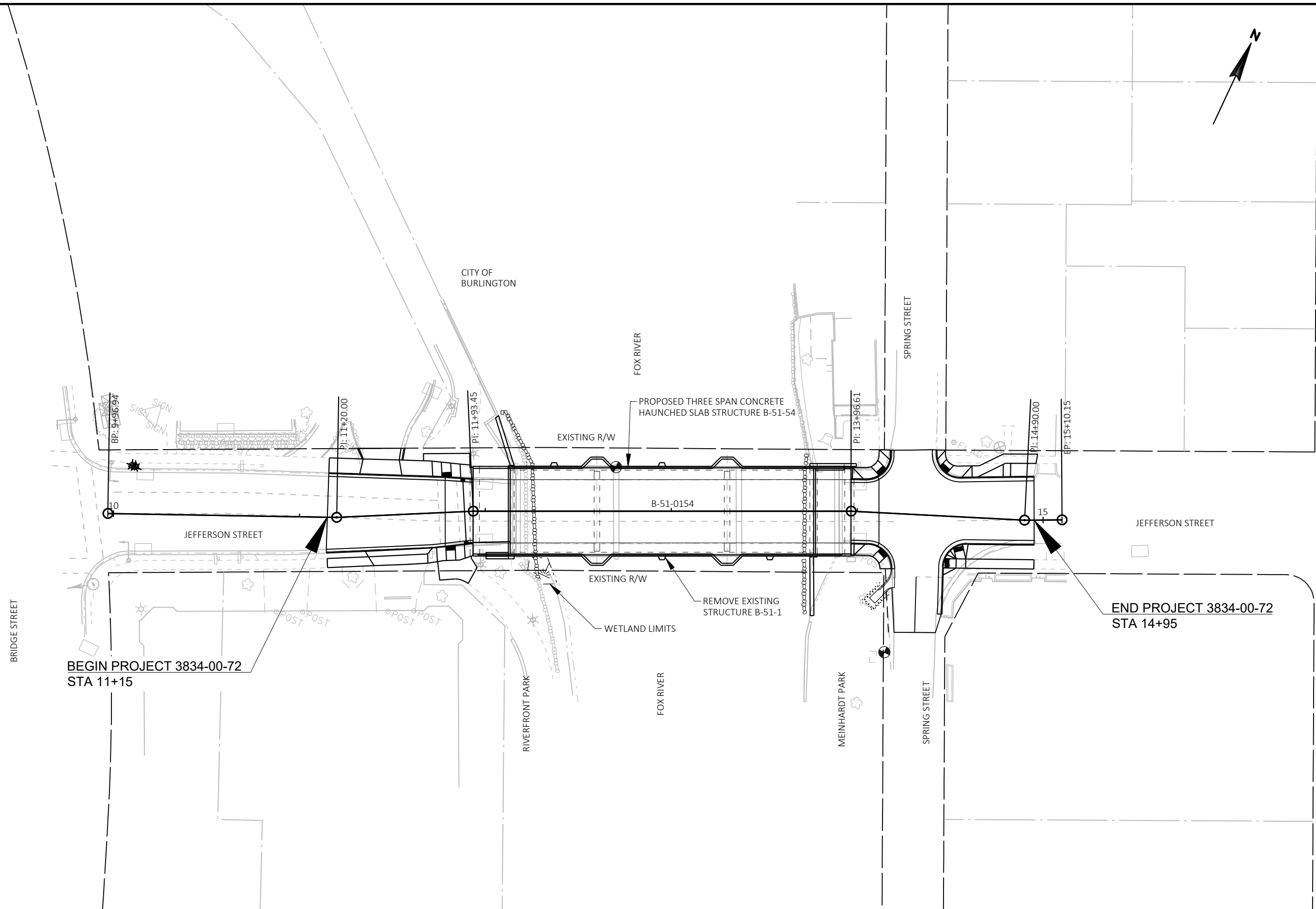
ELECTRIC

WE ENERGIES - ELECTRIC
 SHAYLYNN CONNELLY
 500 S. 116TH STREET
 WEST ALLIS, WI 53214
 PHONE: (414) 944-5926
 EMAIL: SHAYLYN.CONNELLY@WECENERGYGROUP.COM

COMMUNICATION LINE

WINDSTREAM
 AARON GRODI
 PHONE: (608) 819-5014
 EMAIL: AARON.GRODI@WINDSTREAM.COM





PROJECT NO: 3834-00-72

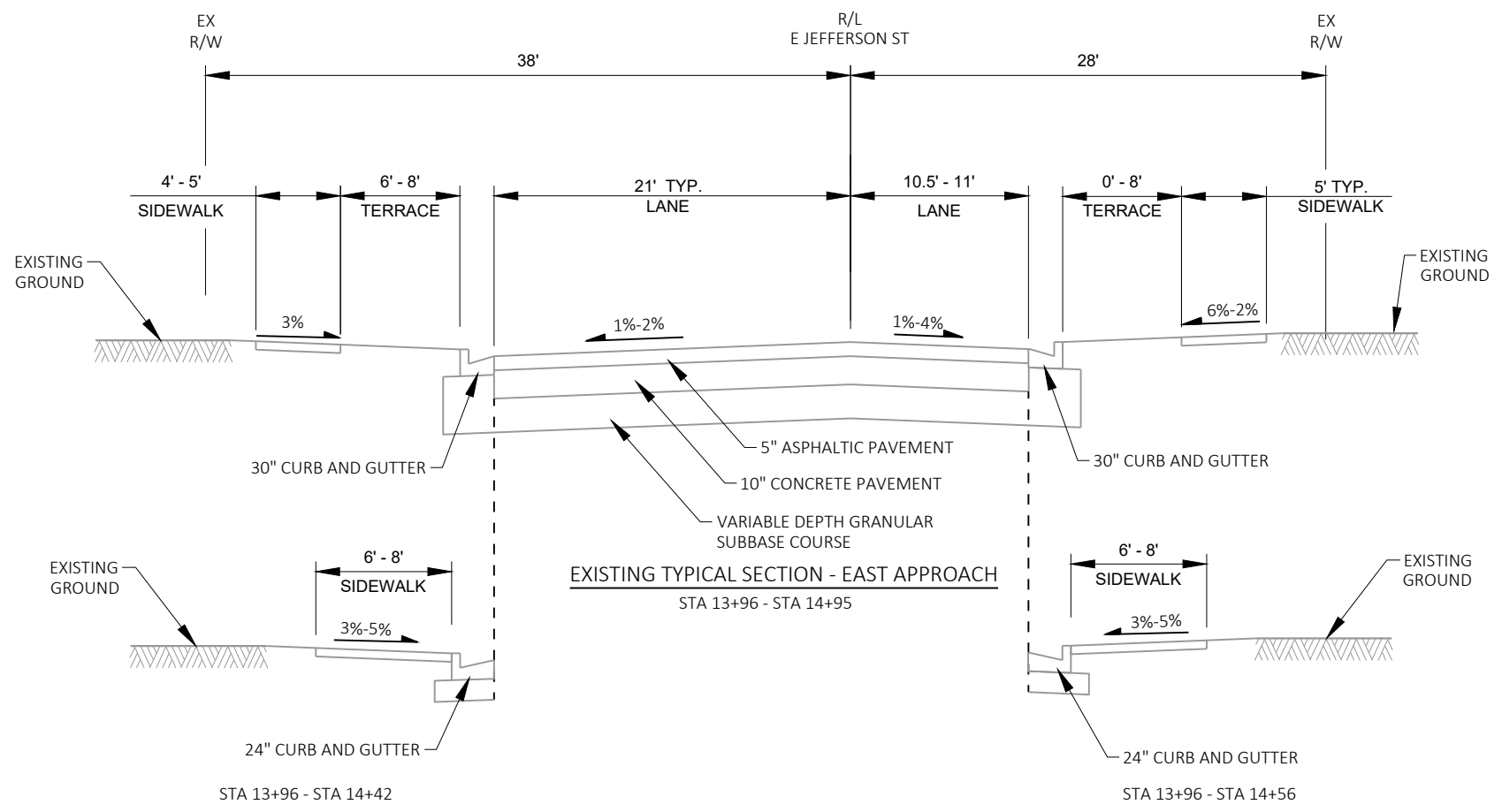
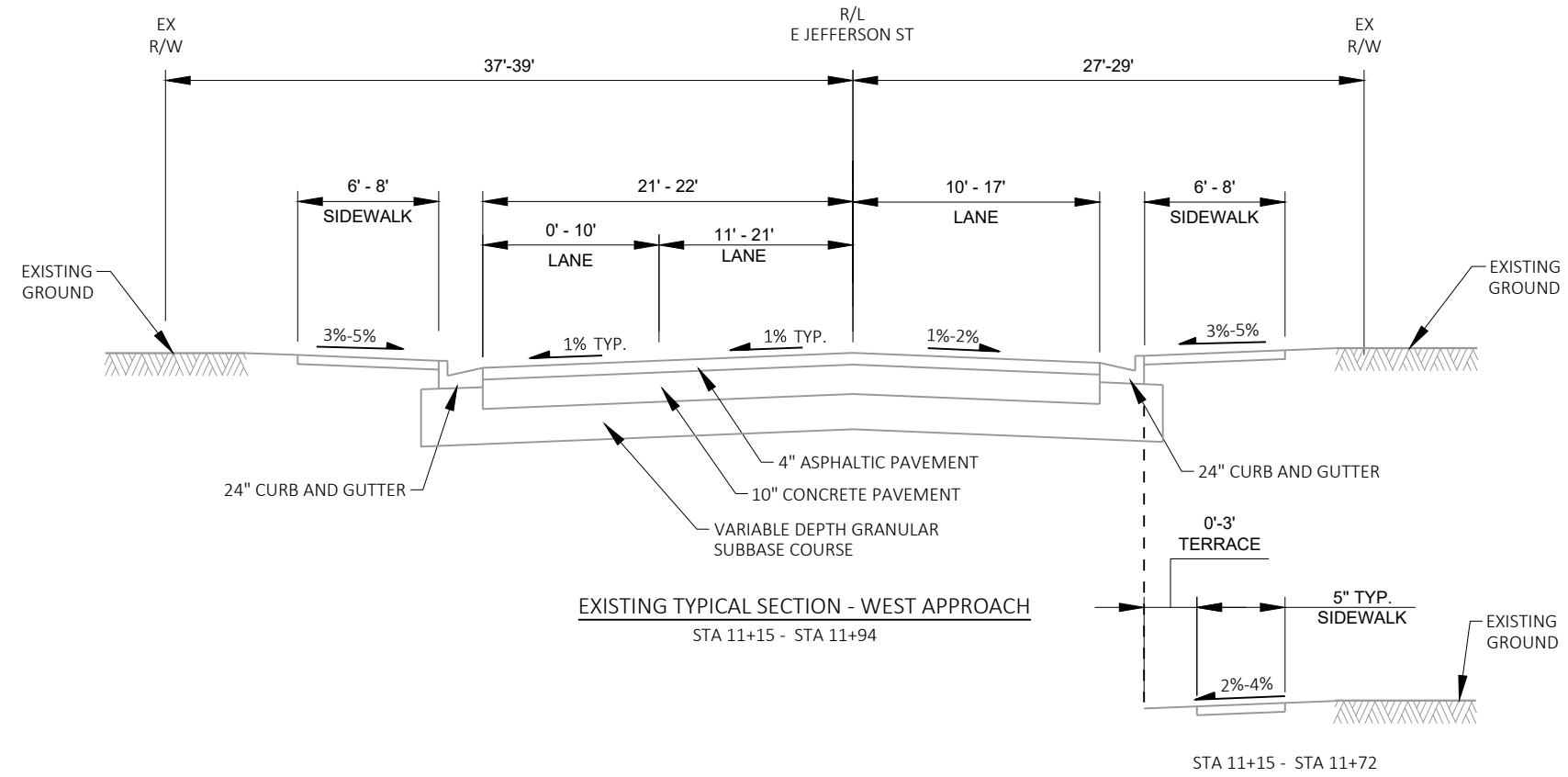
HWY: JEFFERSON STREET

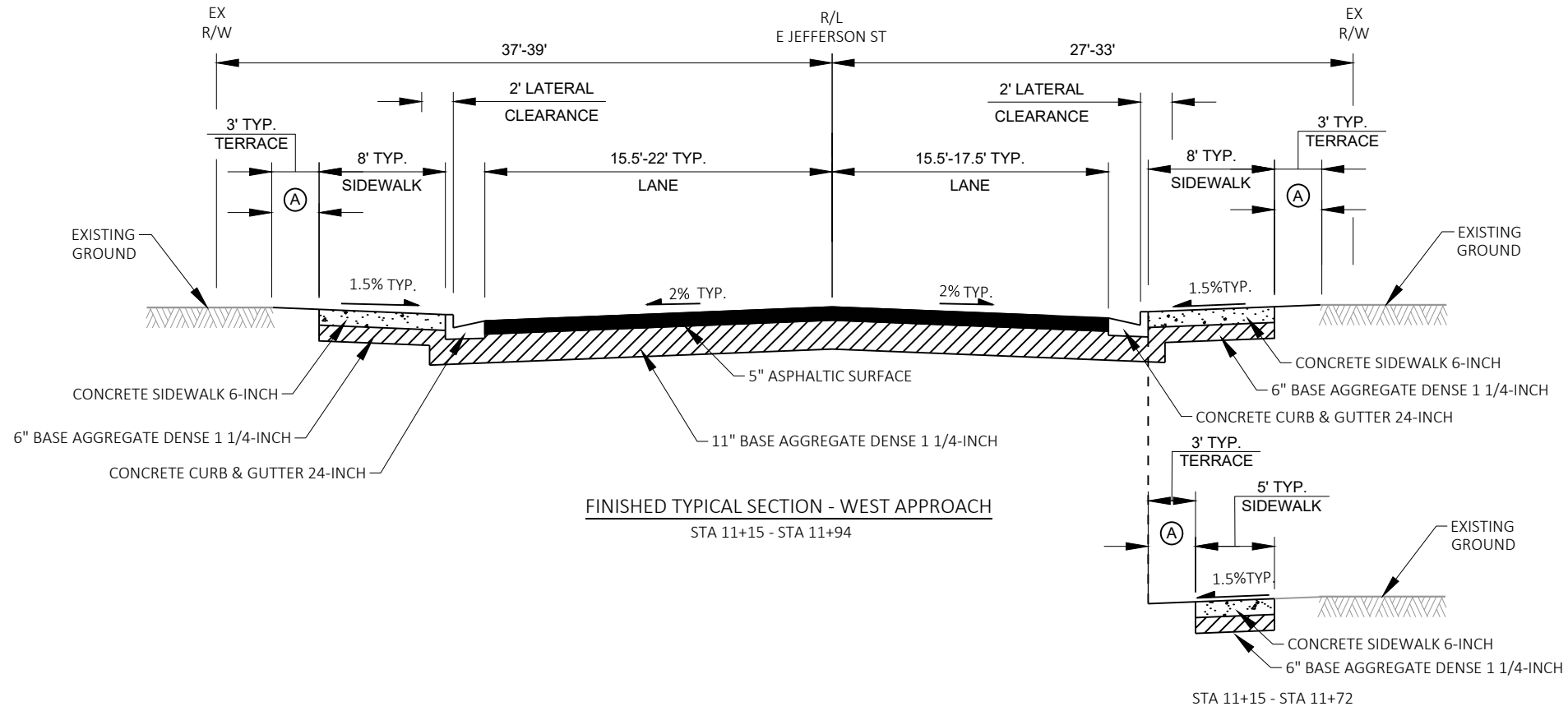
COUNTY: RACINE

PROJECT OVERVIEW

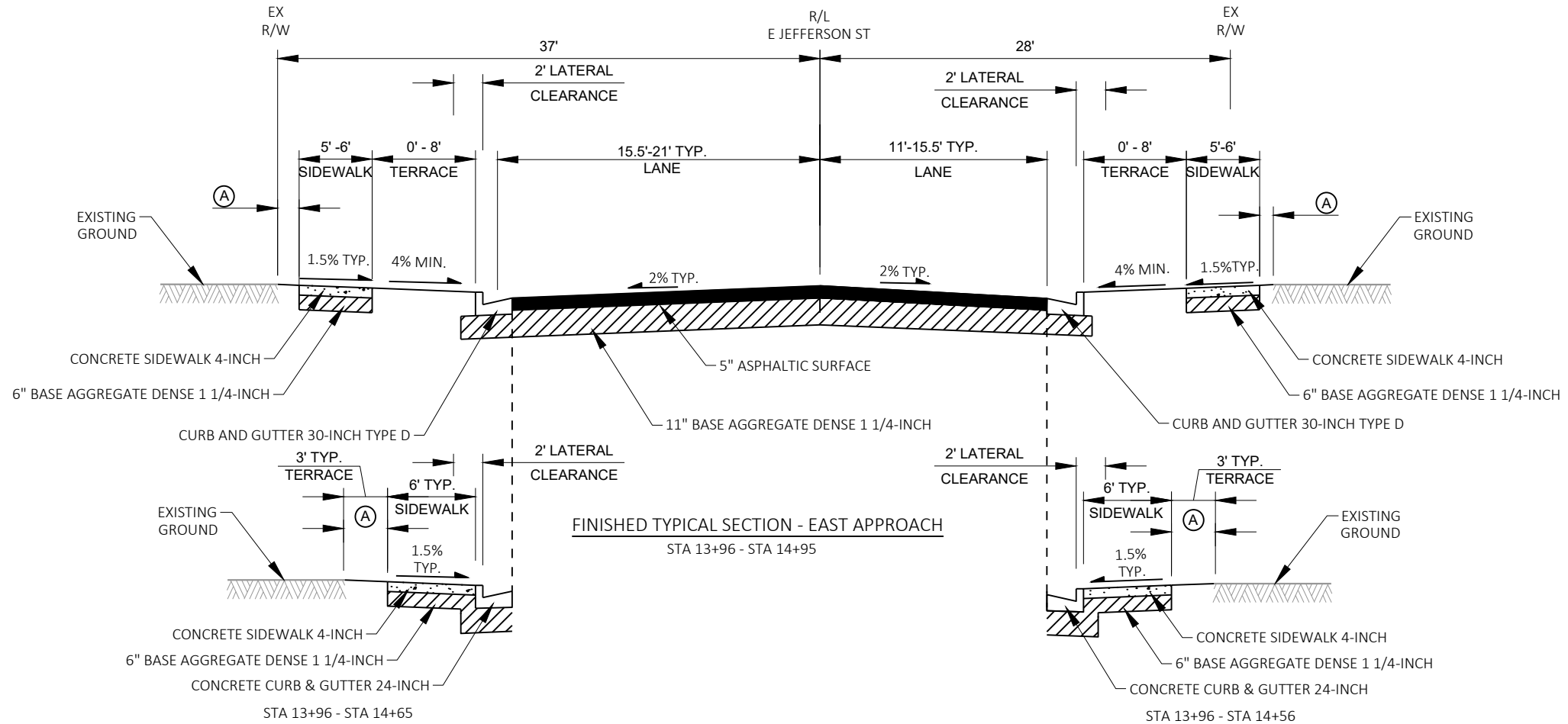
SHEET

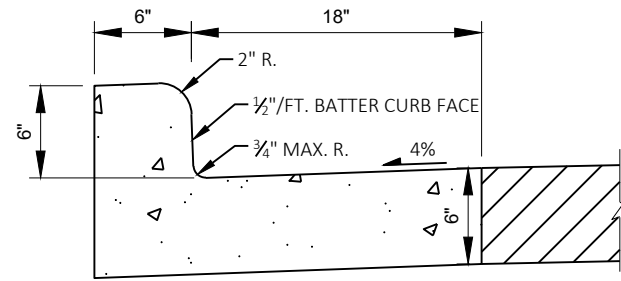
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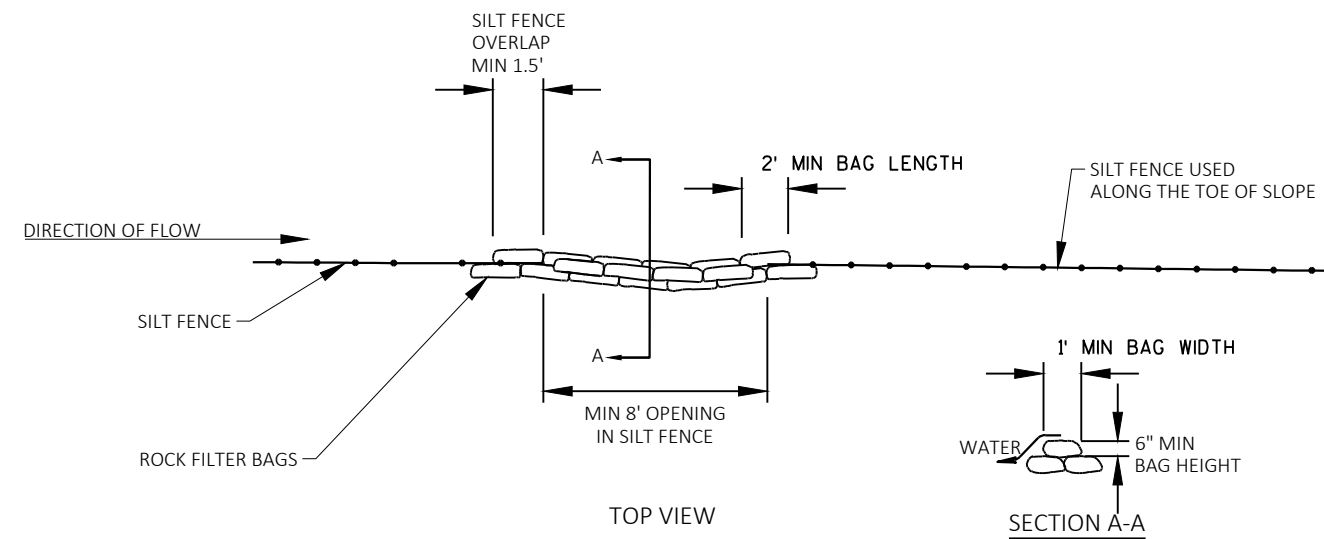


(A) TOPSOIL, TOPSOIL, SOD LAWN, SOD WATER & FERTILIZER TYPE B





CONCRETE CURB AND GUTTER 24-INCH, TYPE A* & D
 *TIE BARS ARE REQUIRED FOR CURB AND GUTTER TYPE A



ROCK BAGS USED FOR SILT FENCE RELIEF

STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
60	11+72.62	17.77 LT	168943.524	508415.449	760.50
61	11+77.62	17.38 LT	168945.490	508420.067	760.63
62	11+82.63	16.84 LT	168947.334	508424.749	760.70
63	11+87.63	16.25 LT	168949.126	508429.459	760.55
64	11+72.60	19.78 LT	168945.295	508414.504	760.94
65	11+77.61	18.88 LT	168946.818	508419.358	760.57
66	11+82.61	18.36 LT	168948.669	508424.038	760.64
67	11+87.62	18.26 LT	168950.902	508428.513	760.99
68	11+72.53	27.92 LT	168952.479	508410.674	761.06
69	11+77.54	27.18 LT	168954.142	508415.454	760.70
70	11+82.55	26.41 LT	168955.778	508420.250	760.76
71	11+87.55	25.67 LT	168957.436	508425.032	761.10

- LEGEND**
- (SW4) CONCRETE SIDEWALK 4-INCH
 - (SW6) CONCRETE SIDEWALK 6-INCH
 - (30D) CONCRETE CURB AND GUTTER 30-INCH, TYPE D
 - (24D) CONCRETE CURB AND GUTTER 24-INCH, TYPE D
 - (24A) CONCRETE CURB AND GUTTER 24-INCH, TYPE A
 - (SOD) RESTORE WITH TOPSOIL, SOD, WATER AND FERTILIZER TYPE B
 - (Grid Pattern) DETECTABLE WARNING FIELD

NOTES:

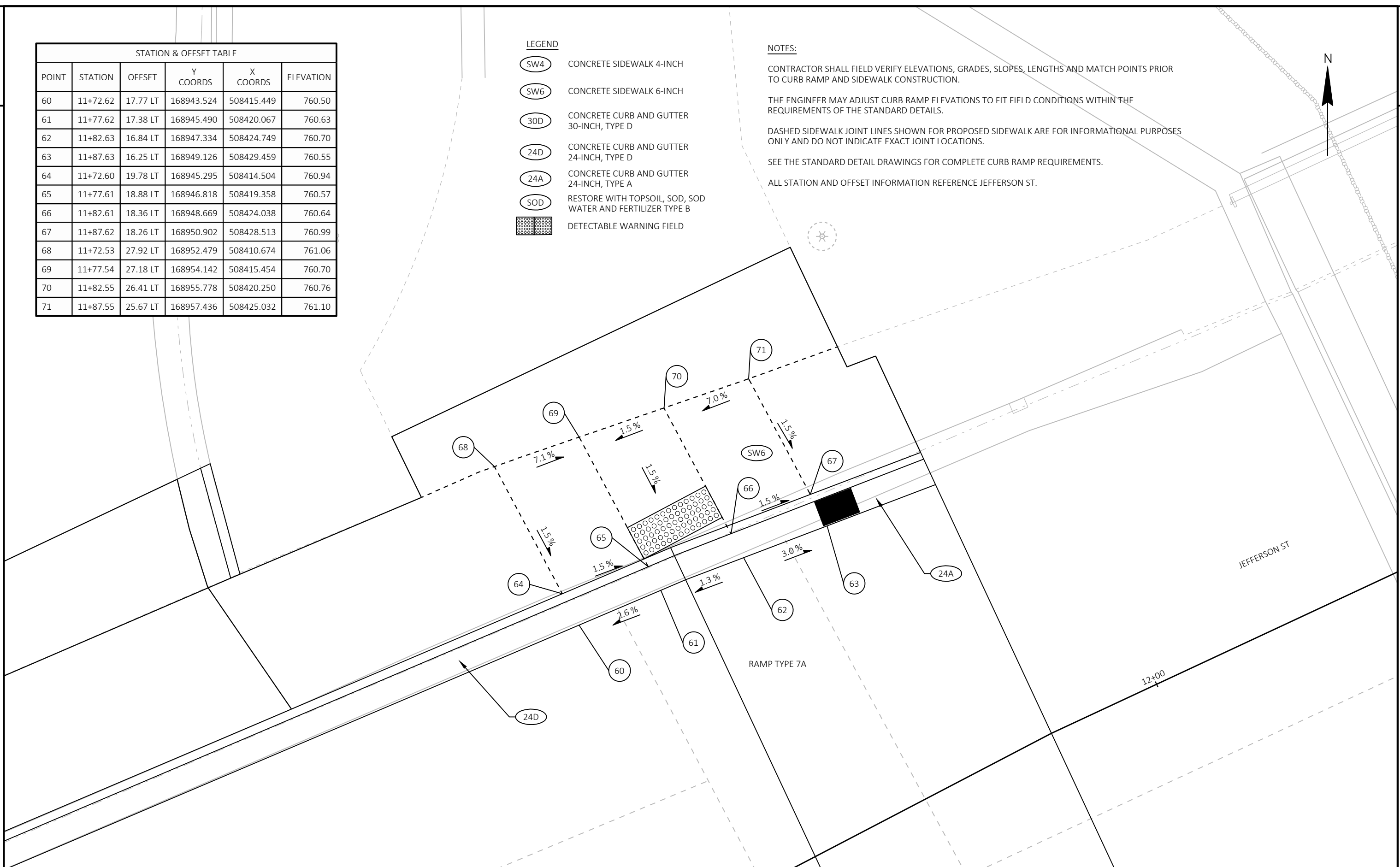
CONTRACTOR SHALL FIELD VERIFY ELEVATIONS, GRADES, SLOPES, LENGTHS AND MATCH POINTS PRIOR TO CURB RAMP AND SIDEWALK CONSTRUCTION.

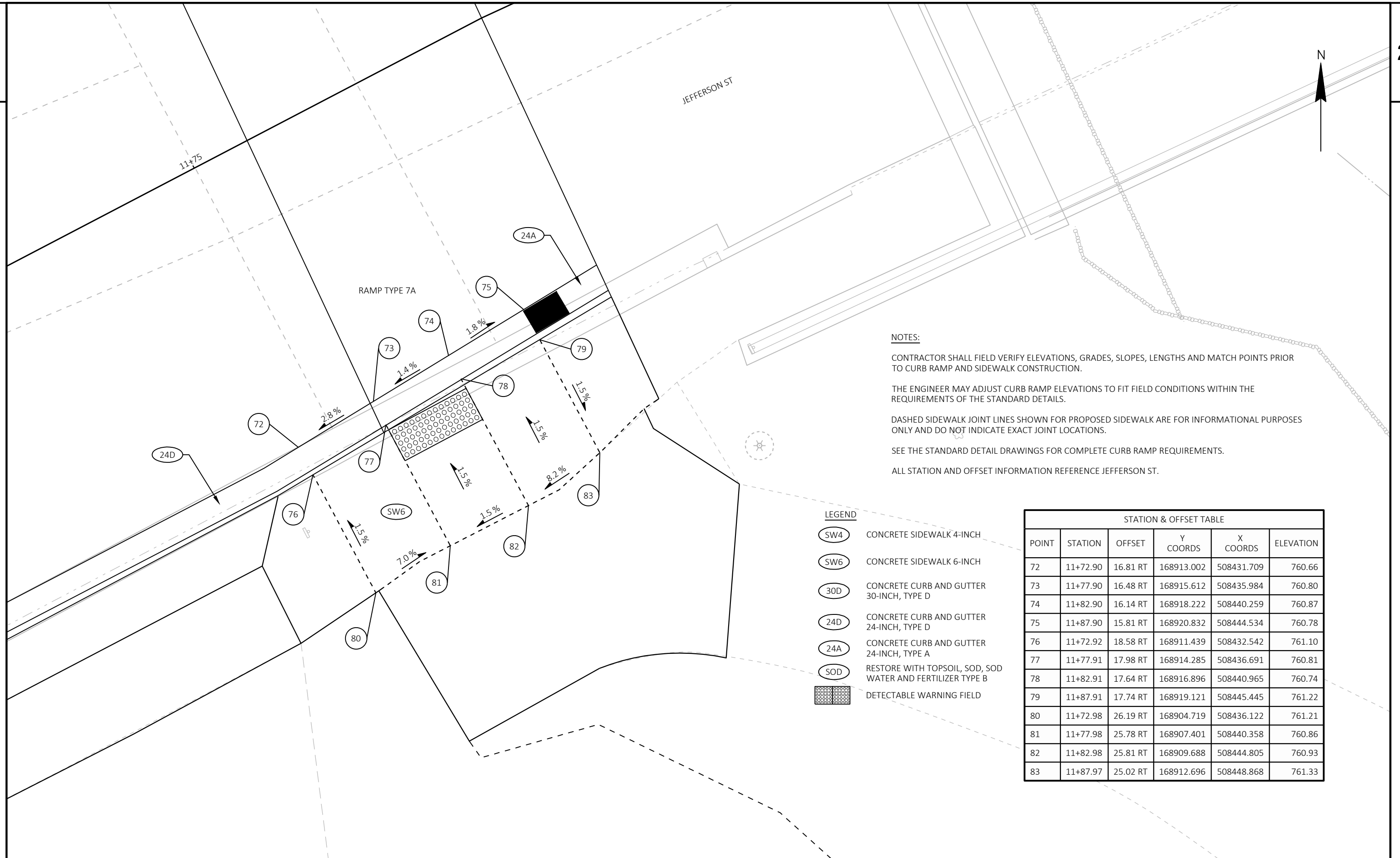
THE ENGINEER MAY ADJUST CURB RAMP ELEVATIONS TO FIT FIELD CONDITIONS WITHIN THE REQUIREMENTS OF THE STANDARD DETAILS.

DASHED SIDEWALK JOINT LINES SHOWN FOR PROPOSED SIDEWALK ARE FOR INFORMATIONAL PURPOSES ONLY AND DO NOT INDICATE EXACT JOINT LOCATIONS.

SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS.

ALL STATION AND OFFSET INFORMATION REFERENCE JEFFERSON ST.





NOTES:

CONTRACTOR SHALL FIELD VERIFY ELEVATIONS, GRADES, SLOPES, LENGTHS AND MATCH POINTS PRIOR TO CURB RAMP AND SIDEWALK CONSTRUCTION.

THE ENGINEER MAY ADJUST CURB RAMP ELEVATIONS TO FIT FIELD CONDITIONS WITHIN THE REQUIREMENTS OF THE STANDARD DETAILS.

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ALL STATION AND OFFSET INFORMATION REFERENCE JEFFERSON ST.

- LEGEND**
- (SW4) CONCRETE SIDEWALK 4-INCH
 - (SW6) CONCRETE SIDEWALK 6-INCH
 - (30D) CONCRETE CURB AND GUTTER 30-INCH, TYPE D
 - (24D) CONCRETE CURB AND GUTTER 24-INCH, TYPE D
 - (24A) CONCRETE CURB AND GUTTER 24-INCH, TYPE A
 - (SOD) RESTORE WITH TOPSOIL, SOD, SOD WATER AND FERTILIZER TYPE B
 - [Hatched Box] DETECTABLE WARNING FIELD

STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
72	11+72.90	16.81 RT	168913.002	508431.709	760.66
73	11+77.90	16.48 RT	168915.612	508435.984	760.80
74	11+82.90	16.14 RT	168918.222	508440.259	760.87
75	11+87.90	15.81 RT	168920.832	508444.534	760.78
76	11+72.92	18.58 RT	168911.439	508432.542	761.10
77	11+77.91	17.98 RT	168914.285	508436.691	760.81
78	11+82.91	17.64 RT	168916.896	508440.965	760.74
79	11+87.91	17.74 RT	168919.121	508445.445	761.22
80	11+72.98	26.19 RT	168904.719	508436.122	761.21
81	11+77.98	25.78 RT	168907.401	508440.358	760.86
82	11+82.98	25.81 RT	168909.688	508444.805	760.93
83	11+87.97	25.02 RT	168912.696	508448.868	761.33

STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
1	14+18.06	33.90 LT	169062.695	508633.442	759.83
2	14+17.91	28.45 LT	169057.589	508635.350	760.20
3	14+15.94	28.76 LT	169057.138	508633.401	760.64
4	14+15.56	22.45 LT	169051.145	508635.420	760.63
5	14+14.45	23.50 LT	169051.700	508634.001	760.57
6	14+12.12	18.82 LT	169046.489	508633.597	760.71
7	14+10.99	19.89 LT	169047.055	508632.152	760.65
8	14+02.42	15.76 LT	169040.010	508625.752	760.93
9	14+02.33	17.76 LT	169041.830	508624.923	761.37
10	13+96.61	23.52 LT	169044.877	508617.139	761.56
11	14+06.13	24.49 LT	169049.497	508625.913	760.74
12	14+09.57	28.12 LT	169054.153	508627.735	760.66
13	14+10.06	33.80 LT	169059.601	508626.065	760.37

- LEGEND**
- (SW4) CONCRETE SIDEWALK 4-INCH
 - (SW6) CONCRETE SIDEWALK 6-INCH
 - (30D) CONCRETE CURB AND GUTTER 30-INCH, TYPE D
 - (24D) CONCRETE CURB AND GUTTER 24-INCH, TYPE D
 - (24A) CONCRETE CURB AND GUTTER 24-INCH, TYPE A
 - (SOD) RESTORE WITH TOPSOIL, SOD, SOD WATER AND FERTILIZER TYPE B
 - [Grid Pattern] DETECTABLE WARNING FIELD

NOTES:

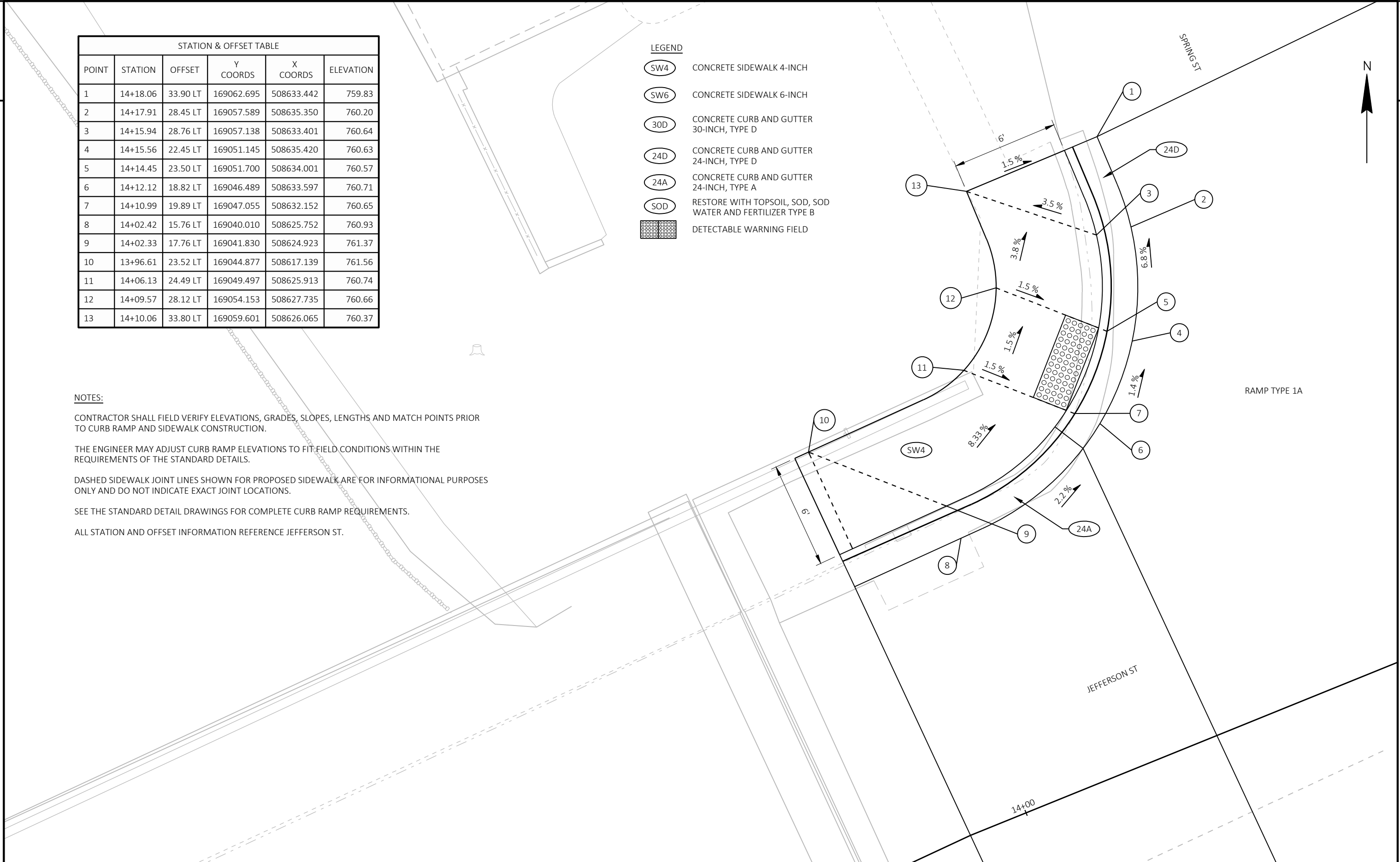
CONTRACTOR SHALL FIELD VERIFY ELEVATIONS, GRADES, SLOPES, LENGTHS AND MATCH POINTS PRIOR TO CURB RAMP AND SIDEWALK CONSTRUCTION.

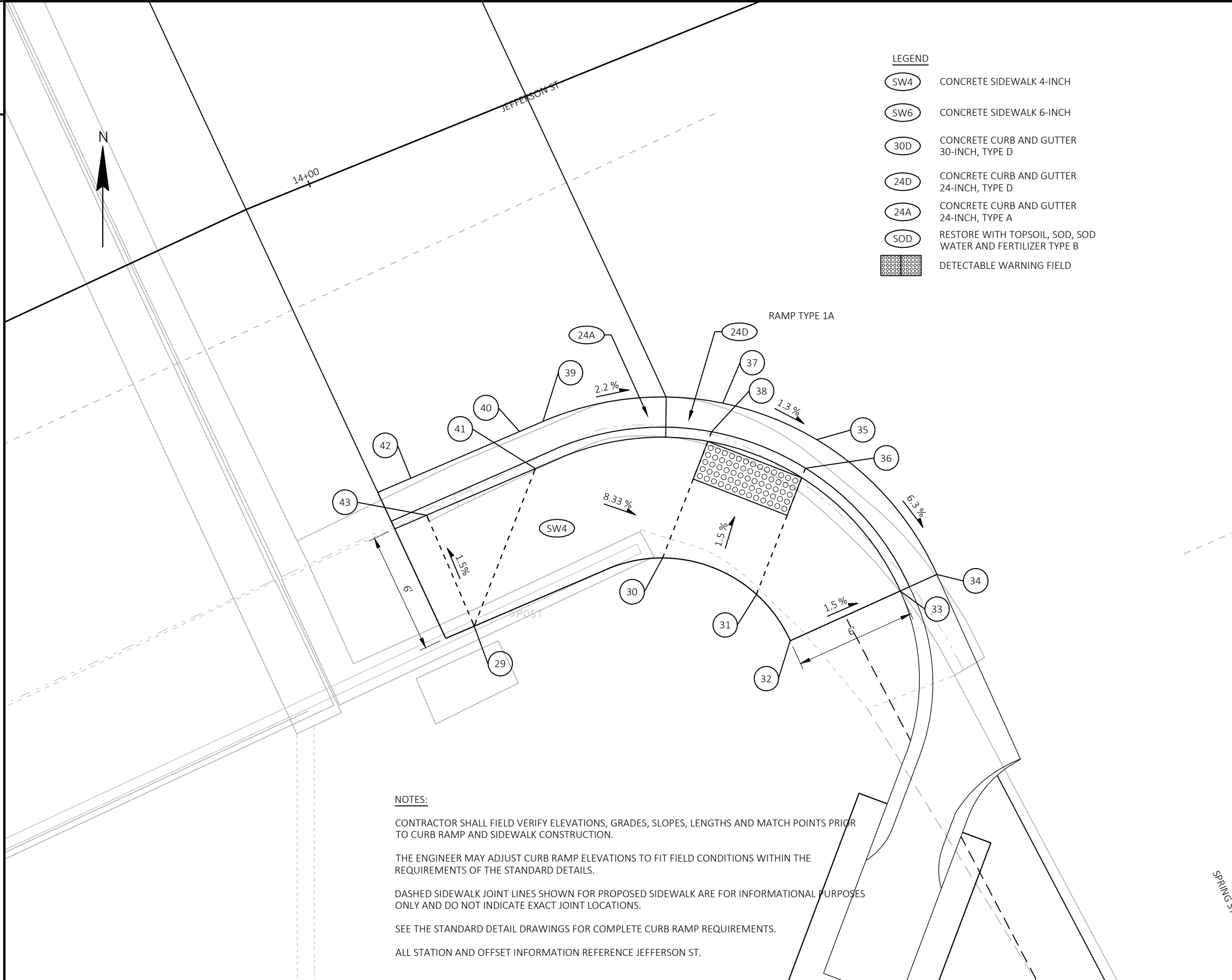
THE ENGINEER MAY ADJUST CURB RAMP ELEVATIONS TO FIT FIELD CONDITIONS WITHIN THE REQUIREMENTS OF THE STANDARD DETAILS.

DASHED SIDEWALK JOINT LINES SHOWN FOR PROPOSED SIDEWALK ARE FOR INFORMATIONAL PURPOSES ONLY AND DO NOT INDICATE EXACT JOINT LOCATIONS.

SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS.

ALL STATION AND OFFSET INFORMATION REFERENCE JEFFERSON ST.





- LEGEND**
- (SW4) CONCRETE SIDEWALK 4-INCH
 - (SW6) CONCRETE SIDEWALK 6-INCH
 - (30D) CONCRETE CURB AND GUTTER 30-INCH, TYPE D
 - (24D) CONCRETE CURB AND GUTTER 24-INCH, TYPE D
 - (24A) CONCRETE CURB AND GUTTER 24-INCH, TYPE A
 - (SOD) RESTORE WITH TOPSOIL, SOD, SOD WATER AND FERTILIZER TYPE B
 - [Grid Pattern] DETECTABLE WARNING FIELD

STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
29	13+99.35	23.44 RT	169002.524	508637.631	761.74
30	14+09.29	23.82 RT	169005.905	508646.988	760.93
31	14+12.93	27.25 RT	169004.096	508651.649	760.85
32	14+13.62	30.01 RT	169001.794	508653.329	760.79
33	14+19.62	29.77 RT	169004.270	508658.794	760.70
34	14+21.62	29.69 RT	169005.095	508660.616	760.26
35	14+18.59	21.24 RT	169011.789	508654.634	760.82
36	14+17.54	22.35 RT	169010.369	508654.083	760.76
37	14+14.95	17.81 RT	169013.598	508649.973	760.89
38	14+13.90	18.92 RT	169012.177	508649.421	760.83
39	14+06.33	15.30 RT	169012.691	508641.041	761.09
40	14+05.07	15.33 RT	169012.193	508639.881	761.11
41	14+05.11	17.33 RT	169010.355	508640.670	761.53
42	13+99.19	15.44 RT	169009.876	508634.478	760.97
43	13+99.23	17.44 RT	169008.038	508635.266	761.65

NOTES:

CONTRACTOR SHALL FIELD VERIFY ELEVATIONS, GRADES, SLOPES, LENGTHS AND MATCH POINTS PRIOR TO CURB RAMP AND SIDEWALK CONSTRUCTION.

THE ENGINEER MAY ADJUST CURB RAMP ELEVATIONS TO FIT FIELD CONDITIONS WITHIN THE REQUIREMENTS OF THE STANDARD DETAILS.

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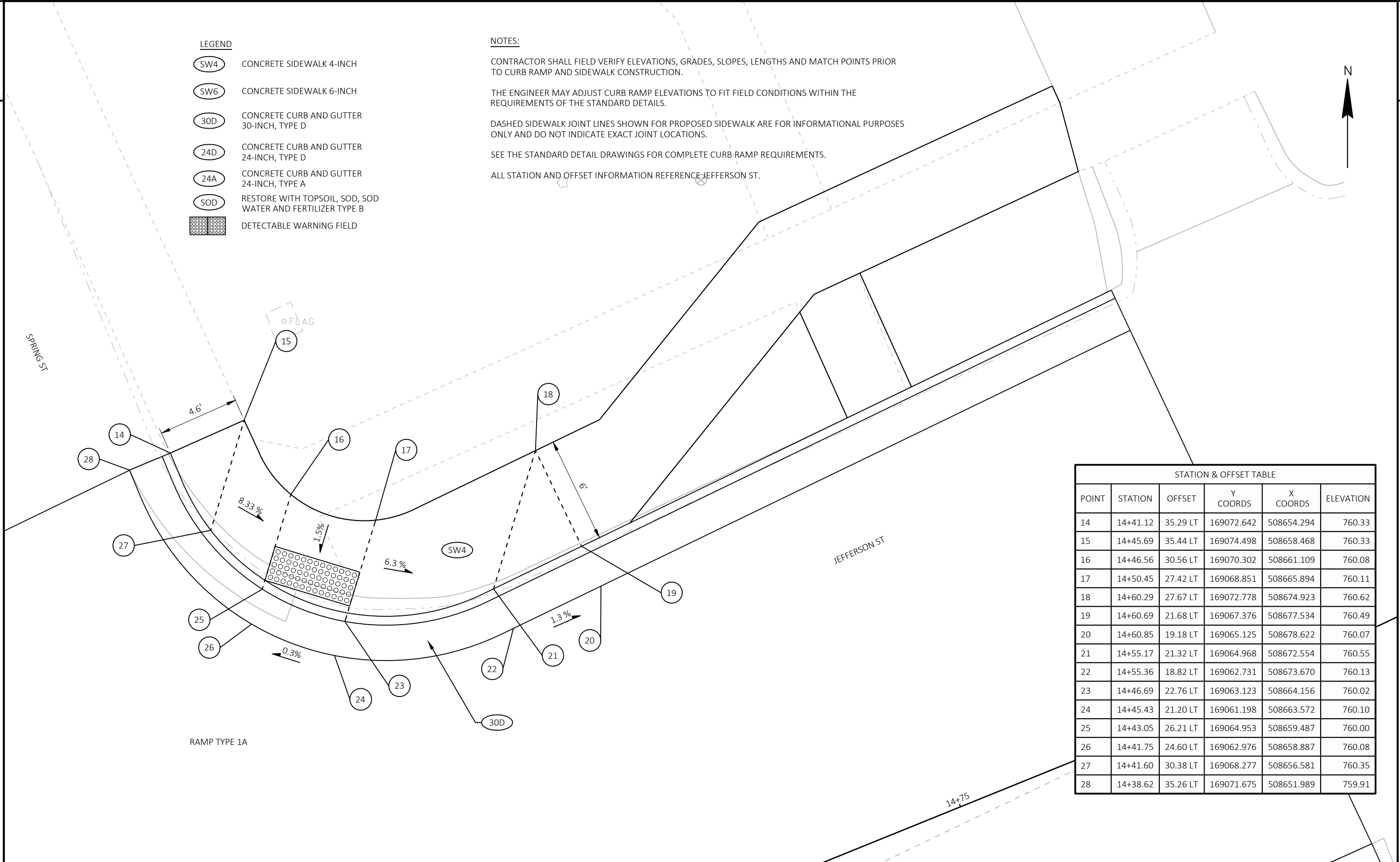
ALL STATION AND OFFSET INFORMATION REFERENCE JEFFERSON ST.

LEGEND

- SW4 CONCRETE SIDEWALK 4-INCH
- SW6 CONCRETE SIDEWALK 6-INCH
- 30D CONCRETE CURB AND GUTTER 30-INCH, TYPE D
- 24D CONCRETE CURB AND GUTTER 24-INCH, TYPE D
- 24A CONCRETE CURB AND GUTTER 24-INCH, TYPE A
- SOD RESTORE WITH TOPSOIL, SOD, WATER AND FERTILIZER TYPE B
- DETECTABLE WARNING FIELD

NOTES:

- CONTRACTOR SHALL FIELD VERIFY ELEVATIONS, GRADES, SLOPES, LENGTHS AND MATCH POINTS PRIOR TO CURB RAMP AND SIDEWALK CONSTRUCTION.
- THE ENGINEER MAY ADJUST CURB RAMP ELEVATIONS TO FIT FIELD CONDITIONS WITHIN THE REQUIREMENTS OF THE STANDARD DETAILS.
- DASHED SIDEWALK JOINT LINES SHOWN FOR PROPOSED SIDEWALK ARE FOR INFORMATIONAL PURPOSES ONLY AND DO NOT INDICATE EXACT JOINT LOCATIONS.
- SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS.
- ALL STATION AND OFFSET INFORMATION REFERENCE JEFFERSON ST.



STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
14	14+41.12	35.29 LT	169072.642	508654.294	760.33
15	14+45.69	35.44 LT	169074.498	508658.468	760.33
16	14+46.56	30.56 LT	169070.302	508661.109	760.08
17	14+50.45	27.42 LT	169068.851	508665.894	760.11
18	14+60.29	27.67 LT	169072.778	508674.923	760.62
19	14+60.69	21.68 LT	169067.376	508677.534	760.49
20	14+60.85	19.18 LT	169065.125	508678.622	760.07
21	14+55.17	21.32 LT	169064.968	508672.554	760.55
22	14+55.36	18.82 LT	169062.731	508673.670	760.13
23	14+46.69	22.76 LT	169063.123	508664.156	760.02
24	14+45.43	21.20 LT	169061.198	508663.572	760.10
25	14+43.05	26.21 LT	169064.953	508659.487	760.00
26	14+41.75	24.60 LT	169062.976	508658.887	760.08
27	14+41.60	30.38 LT	169068.277	508656.581	760.35
28	14+38.62	35.26 LT	169071.675	508651.989	759.91

NOTES:

CONTRACTOR SHALL FIELD VERIFY ELEVATIONS, GRADES, SLOPES, LENGTHS AND MATCH POINTS PRIOR TO CURB RAMP AND SIDEWALK CONSTRUCTION.








THE ENGINEER MAY ADJUST CURB RAMP ELEVATIONS TO FIT FIELD CONDITIONS WITHIN THE REQUIREMENTS OF THE STANDARD DETAILS.

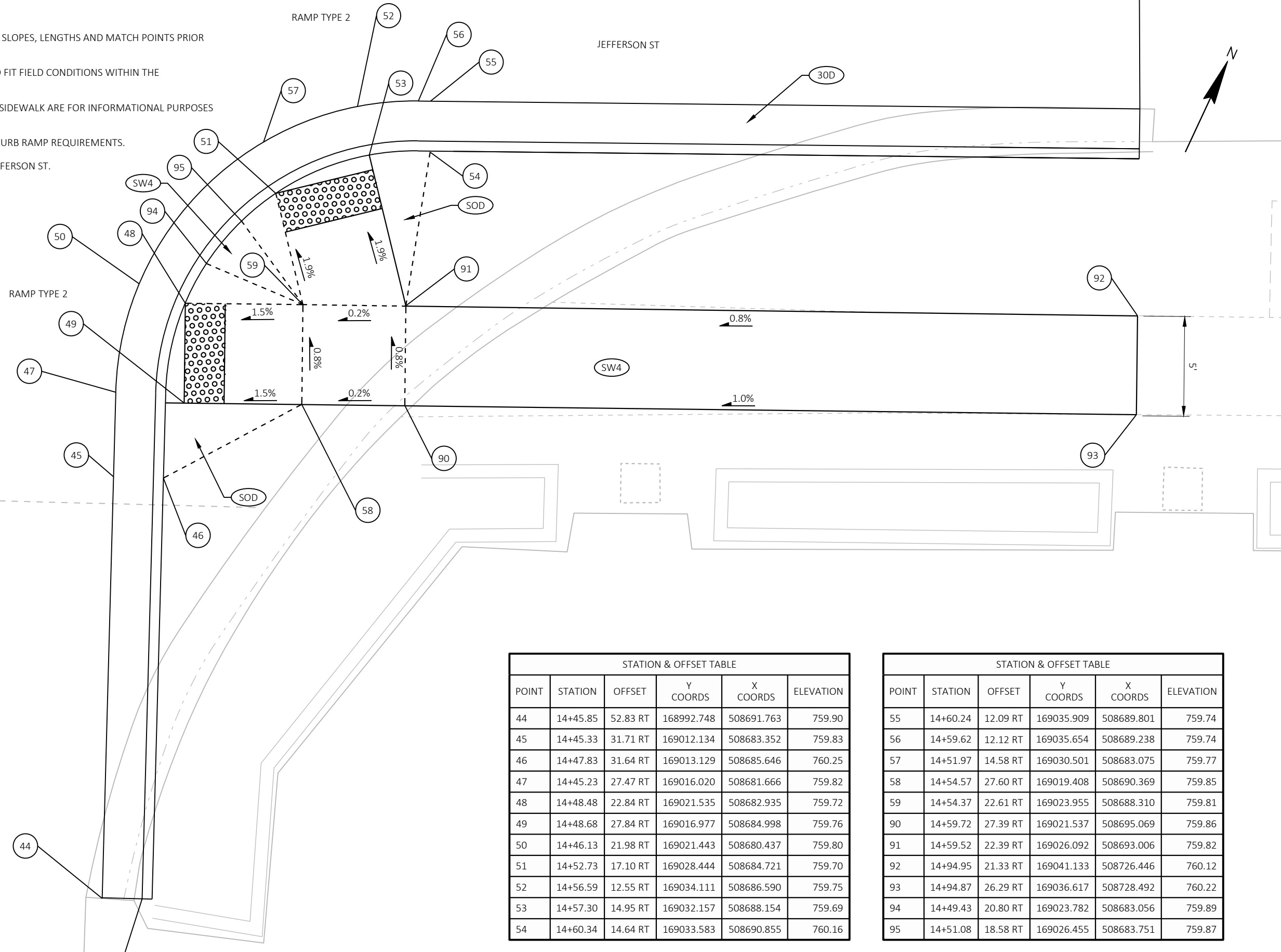
DASHED SIDEWALK JOINT LINES SHOWN FOR PROPOSED SIDEWALK ARE FOR INFORMATIONAL PURPOSES ONLY AND DO NOT INDICATE EXACT JOINT LOCATIONS.

SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS.

ALL STATION AND OFFSET INFORMATION REFERENCE JEFFERSON ST.

LEGEND









-  CONCRETE SIDEWALK 4-INCH
-  CONCRETE SIDEWALK 6-INCH
-  CONCRETE CURB AND GUTTER 30-INCH, TYPE D
-  CONCRETE CURB AND GUTTER 24-INCH, TYPE D
-  CONCRETE CURB AND GUTTER 24-INCH, TYPE A
-  RESTORE WITH TOPSOIL, SOD, SOD WATER AND FERTILIZER TYPE B
-  DETECTABLE WARNING FIELD



STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
44	14+45.85	52.83 RT	168992.748	508691.763	759.90
45	14+45.33	31.71 RT	169012.134	508683.352	759.83
46	14+47.83	31.64 RT	169013.129	508685.646	760.25
47	14+45.23	27.47 RT	169016.020	508681.666	759.82
48	14+48.48	22.84 RT	169021.535	508682.935	759.72
49	14+48.68	27.84 RT	169016.977	508684.998	759.76
50	14+46.13	21.98 RT	169021.443	508680.437	759.80
51	14+52.73	17.10 RT	169028.444	508684.721	759.70
52	14+56.59	12.55 RT	169034.111	508686.590	759.75
53	14+57.30	14.95 RT	169032.157	508688.154	759.69
54	14+60.34	14.64 RT	169033.583	508690.855	760.16

STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
55	14+60.24	12.09 RT	169035.909	508689.801	759.74
56	14+59.62	12.12 RT	169035.654	508689.238	759.74
57	14+51.97	14.58 RT	169030.501	508683.075	759.77
58	14+54.57	27.60 RT	169019.408	508690.369	759.85
59	14+54.37	22.61 RT	169023.955	508688.310	759.81
90	14+59.72	27.39 RT	169021.537	508695.069	759.86
91	14+59.52	22.39 RT	169026.092	508693.006	759.82
92	14+94.95	21.33 RT	169041.133	508726.446	760.12
93	14+94.87	26.29 RT	169036.617	508728.492	760.22
94	14+49.43	20.80 RT	169023.782	508683.056	759.89
95	14+51.08	18.58 RT	169026.455	508683.751	759.87

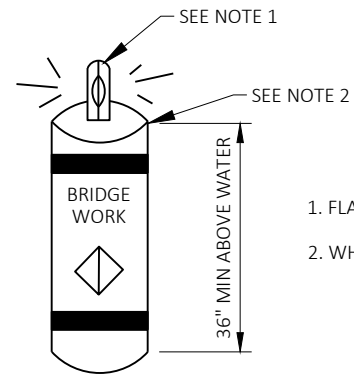
LEGEND

-  INLET PROTECTION, TYPE
-  SURFACE WATER FLOW
-  SILT FENCE
-  SLOPE INTERCEPT
-  TURBIDITY BARRIER
-  SOD LAWN
-  ASPHALTIC FLUME
-  BOATS KEEP OUT BUOY

IMMEDIATE RECEIVING WATER IS FOX RIVER.
 BUOYS TO REMAIN IN THE WATER AT ALL TIMES DURING BRIDGE REMOVAL AND BRIDGE CONSTRUCTION.

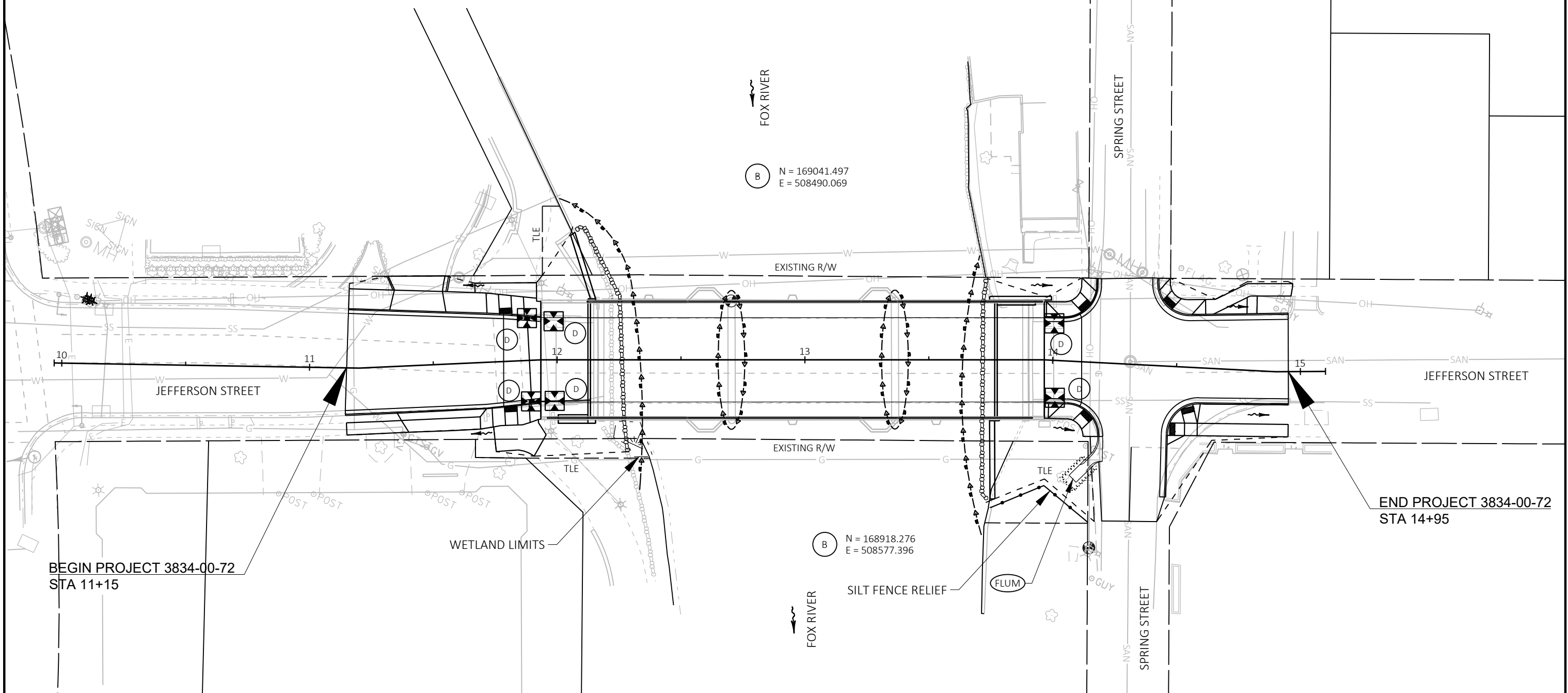
TOTAL PROJECT AREA = 0.634 ACRES.

TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = 0.491 ACRES



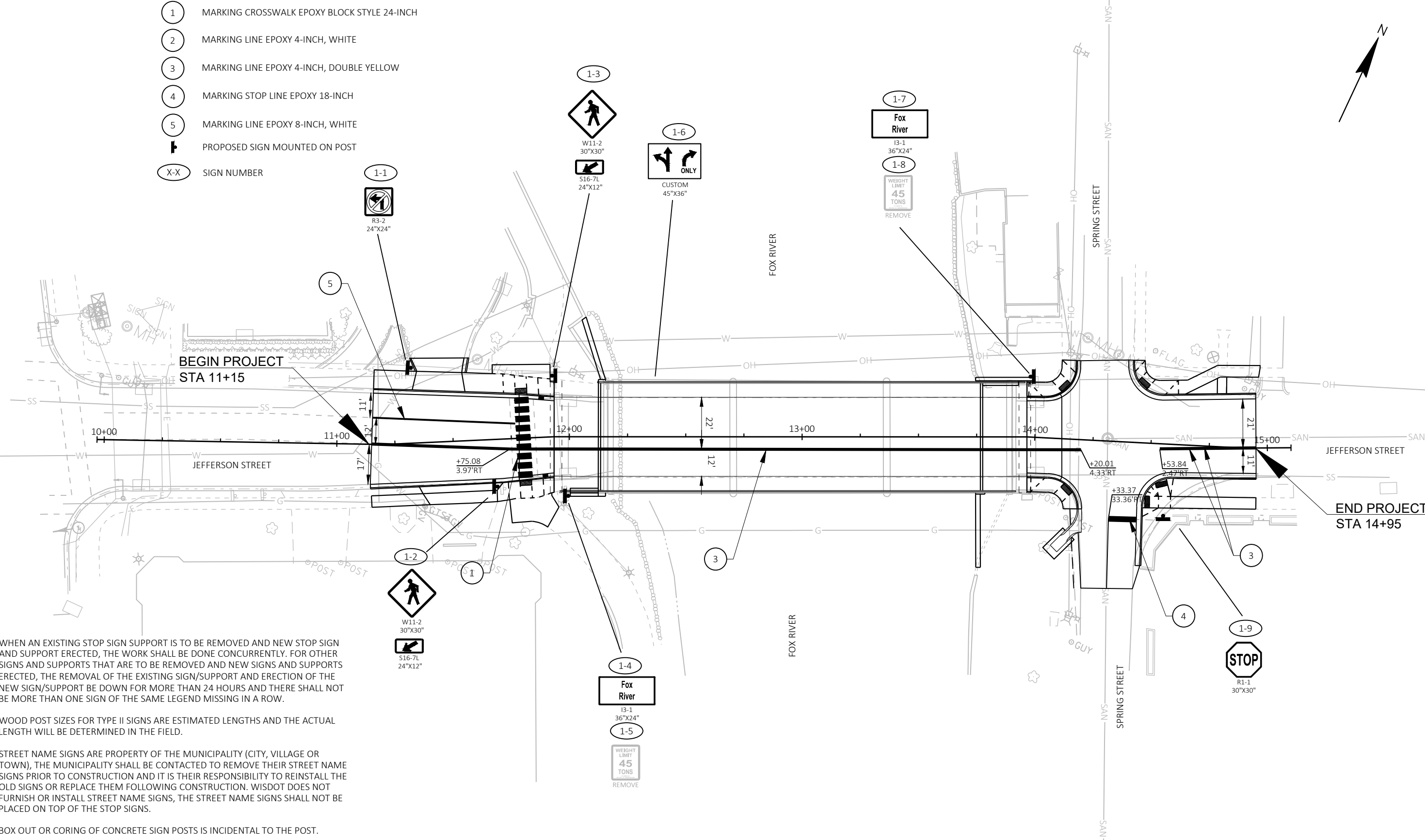
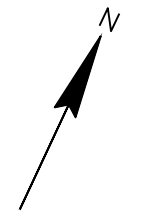
1. FLASHING LIGHT REQUIRED.
2. WHITE BUOY WITH ORANGE MARKING.

TYPICAL KEEP OUT BUOY



LEGEND

- 1 MARKING CROSSWALK EPOXY BLOCK STYLE 24-INCH
- 2 MARKING LINE EPOXY 4-INCH, WHITE
- 3 MARKING LINE EPOXY 4-INCH, DOUBLE YELLOW
- 4 MARKING STOP LINE EPOXY 18-INCH
- 5 MARKING LINE EPOXY 8-INCH, WHITE
- PROPOSED SIGN MOUNTED ON POST
- X-X SIGN NUMBER



WHEN AN EXISTING STOP SIGN SUPPORT IS TO BE REMOVED AND NEW STOP SIGN AND SUPPORT ERECTED, THE WORK SHALL BE DONE CONCURRENTLY. FOR OTHER SIGNS AND SUPPORTS THAT ARE TO BE REMOVED AND NEW SIGNS AND SUPPORTS ERECTED, THE REMOVAL OF THE EXISTING SIGN/SUPPORT AND ERECTION OF THE NEW SIGN/SUPPORT BE DOWN FOR MORE THAN 24 HOURS AND THERE SHALL NOT BE MORE THAN ONE SIGN OF THE SAME LEGEND MISSING IN A ROW.

WOOD POST SIZES FOR TYPE II SIGNS ARE ESTIMATED LENGTHS AND THE ACTUAL LENGTH WILL BE DETERMINED IN THE FIELD.

STREET NAME SIGNS ARE PROPERTY OF THE MUNICIPALITY (CITY, VILLAGE OR TOWN), THE MUNICIPALITY SHALL BE CONTACTED TO REMOVE THEIR STREET NAME SIGNS PRIOR TO CONSTRUCTION AND IT IS THEIR RESPONSIBILITY TO REINSTALL THE OLD SIGNS OR REPLACE THEM FOLLOWING CONSTRUCTION. WISDOT DOES NOT FURNISH OR INSTALL STREET NAME SIGNS, THE STREET NAME SIGNS SHALL NOT BE PLACED ON TOP OF THE STOP SIGNS.

BOX OUT OR CORING OF CONCRETE SIGN POSTS IS INCIDENTAL TO THE POST.

PROJECT NO: 3834-00-72	HWY: JEFFERSON STREET	COUNTY: RACINE	PAVEMENT MARKING & PERMANENT SIGNING	SHEET E
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MATCHLINE

MATCHLINE









EXISTING BUILDING

EQUIPMENT ROOM

ADD 20A/2P (CKT. 11/13) AND 20A/1P (CKT. 15) CIRCUIT BREAKERS IN EXISTING LEFT SIDE SQUARE D QO LOAD CENTER

TRANSITION NEW 8AWG UNDERGROUND CONDUCTORS AT EXISTING PULL BOX TO 12AWG (XHHW) AND ROUTE PHASE CONDUCTORS THROUGH SPARE CONTACTS IN EXISTING CONTACTOR (ON RIGHT) AND THEN TO NEW CIRCUIT BREAKERS/EXISTING LOAD CENTER AS APPROPRIATE. EXTEND NEUTRAL CONDUCTOR TO NEUTRAL BAR.

LEGEND:

-  EXISTING LUMINAIRE
 -  EXISTING RIVERWALK LIGHTING UNIT
 -  NEW DECORATIVE LIGHTING UNIT, LED LUMINAIRE
 -  EXISTING PULL BOX 11x18x12-INCH
 -  NEW PULL BOX COMPOSITE 17x30x36-INCH
 -  NEW JUNCTION BOX (INSTALLED AS PART OF BRIDGE)
 -  PROPOSED CONDUCTORS IN CONDUIT: 2-8 AWG LIGHTING (240V), 2-8 AWG RECEPTACLE (120V) & 1-8 AWG GND IN 2-INCH SCHEDULE 40 NONMETALLIC CONDUIT
 -  EXISTING CONDUIT/CONDUCTORS
- STA. 6+00.0, 20.00'RT ← STATION/OFFSET (LOCATION TO CENTER OF BASE/PULL BOX)
- BL-1/3L-5R-1 OR LPB-1
- NUMBER LIGHTING PULL BOX
 - POLE NUMBER
 - RECEPTACLE CIRCUIT NUMBER
 - LUMINAIRE CIRCUIT NUMBER(S)
 - POWER (SOURCE)

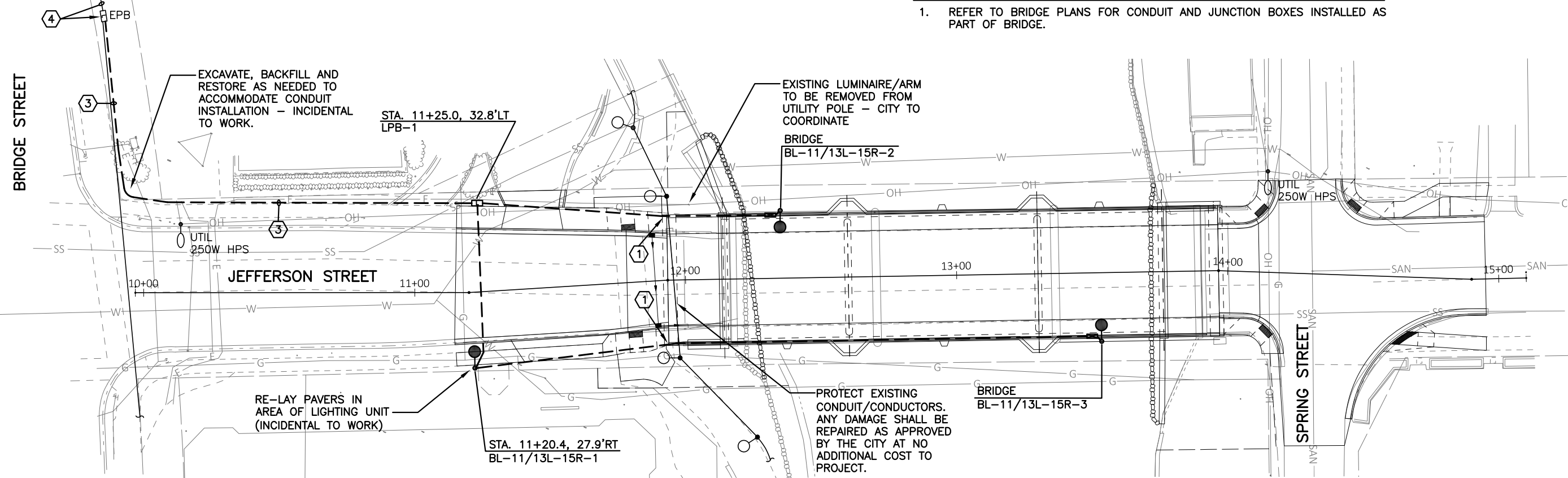


KEYED NOTES:

- ① CONNECT TO CONDUIT INSTALLED AS PART OF BRIDGE.
- ② NO SPLICES ALLOWED IN PULL BOX, EXCEPT FOR GROUNDING CONNECTION.
- ③ DIRECTIONAL BORE CONDUIT.
- ④ CONNECT NEW CONDUIT TO EXISTING PULL BOX. ROUTE NEW CONDUCTORS (2-8 AWG LIGHTING AND 2-8 AWG RECEPTACLE) THROUGH EXISTING 2-INCH CONDUIT (THAT CURRENTLY HAS 2-4 AWG & 3-6 AWG AND 1-6 AWG GND) TO EXISTING BUILDING - REMOVE/REINSTALL EXISTING CONDUCTORS AS NEEDED. RESTORE AREA AROUND PULL BOX AS NEEDED.

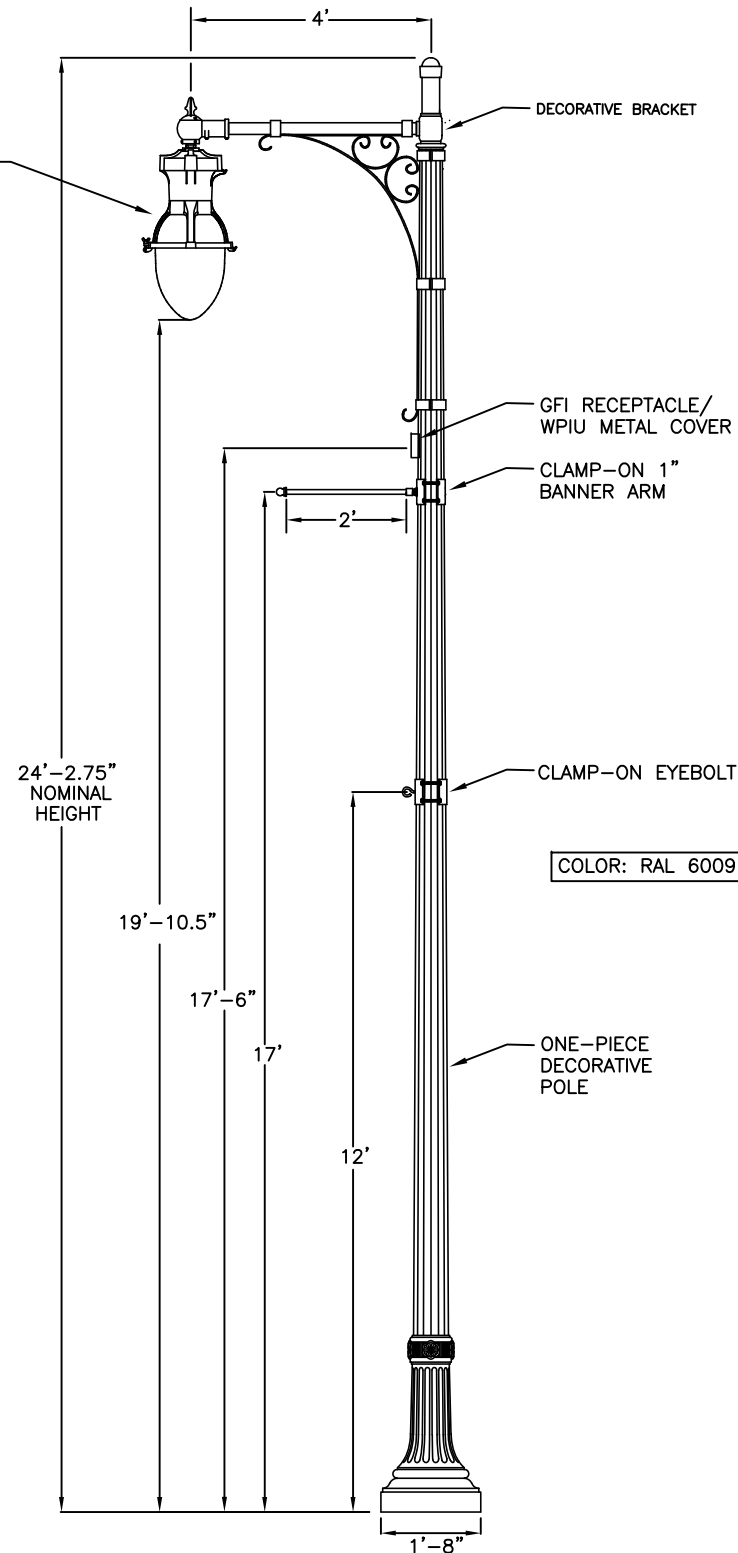
GENERAL NOTE:

1. REFER TO BRIDGE PLANS FOR CONDUIT AND JUNCTION BOXES INSTALLED AS PART OF BRIDGE.



DESIGNED BY: POWRTEK ENGINEERING, INC. SHEET 1 OF 3

LED LUMINAIRE, 130W,
13,000 NOM. LUMENS, TYPE
3 DISTRIBUTION, 4000K,
70CRI, 120-277V
(CONNECTED AT 240V)



LIGHTING UNIT NOTE:

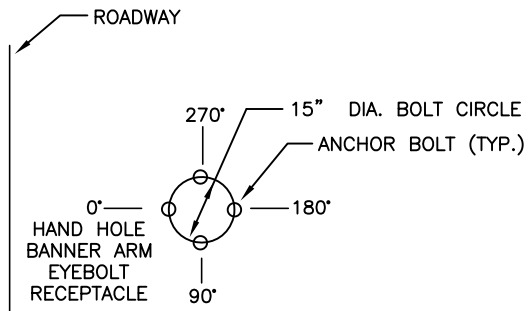
VERIFY WITH OWNER IF BANNER ARM AND EYEBOLT ARE TO BE INSTALLED PRIOR TO INSTALLATION, OTHERWISE TURN OVER TO OWNER.

DECORATIVE LIGHTING UNIT DETAIL

NO SCALE

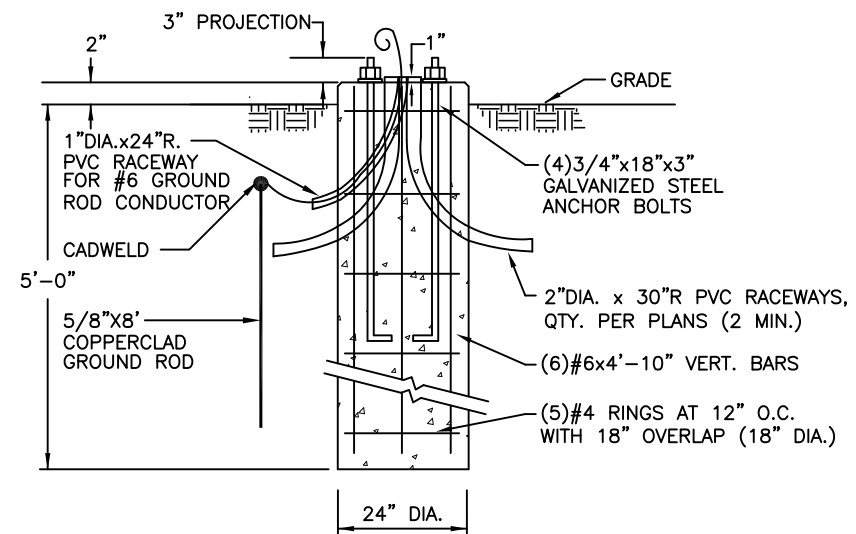
CONCRETE BASE NOTES:

1. FORM TOP 12" OF BASE. REMOVE FORM AFTER SEVEN DAYS.
2. COORDINATE EXACT ANCHOR BOLT INSTALLATION REQUIREMENTS WITH SUPPLIED POLE. POLE TO ESSENTIALLY SIT DIRECTLY ON CONCRETE BASE. SHIMS (OR WASHERS) TO BE USED FOR PLUMBING POLE. NO GROUT SHALL BE USED.
3. PROVIDE 1/2" EXPANSION JOINT MATERIAL AROUND BASE WHERE INSTALLED IN PAVEMENT/PAVERS.
4. CONDUITS AND ANCHOR BOLTS SHALL BE CENTERED ON CONCRETE BASE.



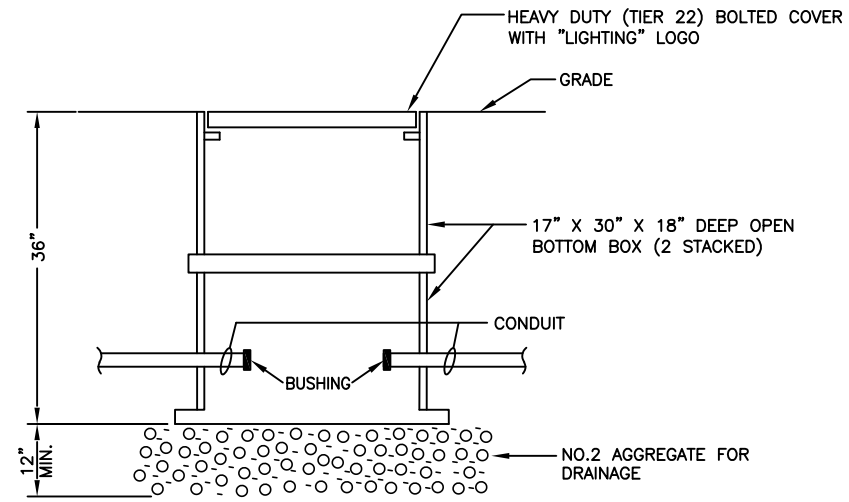
BOLT PATTERN/POLE ORIENTATION DETAIL

NO SCALE



DECORATIVE LIGHTING UNIT CONCRETE BASE

NO SCALE

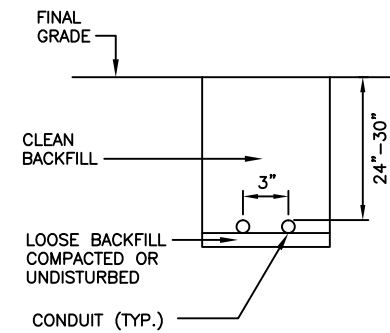


NOTES

1. PULLBOX IS A CONCRETE COMPOSITE, UL LISTED, QUAZITE PG SERIES.
2. SHIM BETWEEN SECTIONS AS REQUIRED SO BOX IS LEVEL WITH FINAL GRADE.
3. SEAL ALL OPENINGS IN BOX FOR DUCT/CONDUIT.
4. WHERE SPLICES OCCUR IN PULL BOX PER PLANS, PROVIDE MULTI-CABLE COMPRESSION CONNECTOR ENCASED IN SCOTCHCAST 85 SERIES MULTI-MOLD PERMANENT RESIN COMPOUND.
5. LEAVE 6-FT OF SLACK IN CONDUCTORS IN PULL BOX.
6. ALL CONDUCTORS TO HAVE A CIRCUIT TAG AS DESCRIBED UNDER POLE WIRING DETAIL.

PULL BOX DETAIL

NO SCALE

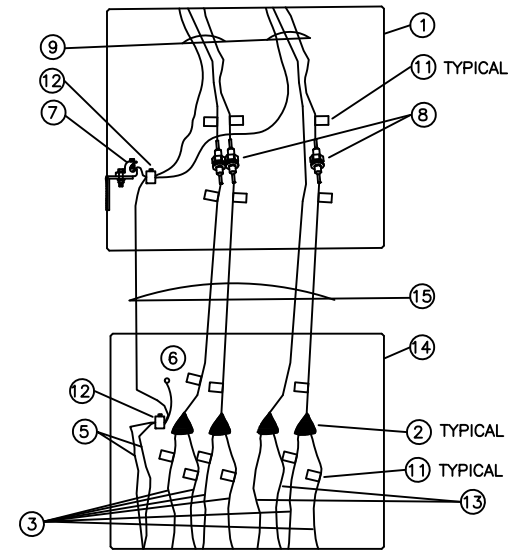


NOTE:

1. CONDUIT SHALL BE TRENCHED OR DIRECTIONAL BORED AS REQUIRED.

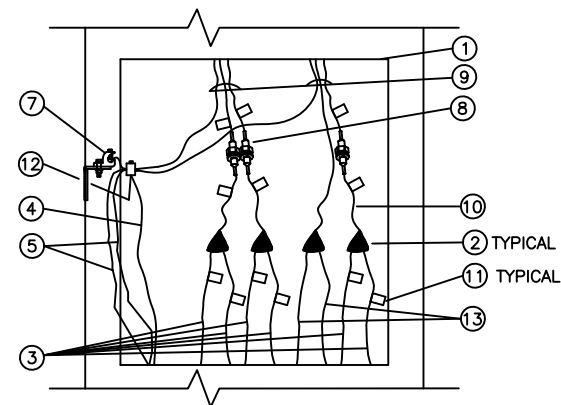
TRENCH FOR CONDUIT DETAIL

NO SCALE



BRIDGE MOUNTED POLE WIRING DETAIL

NO SCALE



GROUND MOUNTED POLE WIRING DETAIL

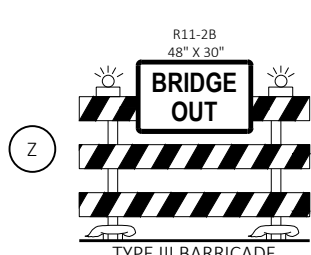
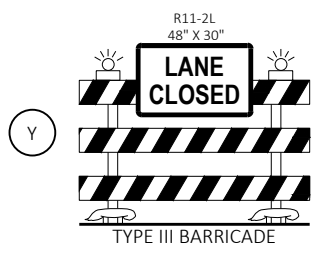
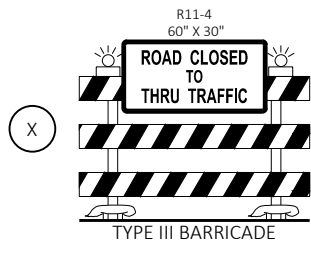
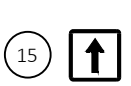
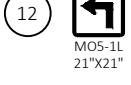
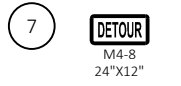
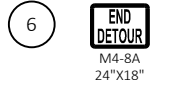
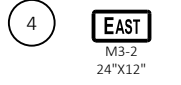
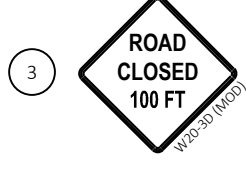
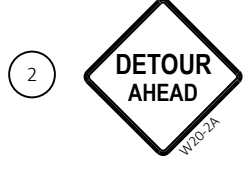
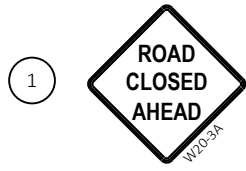
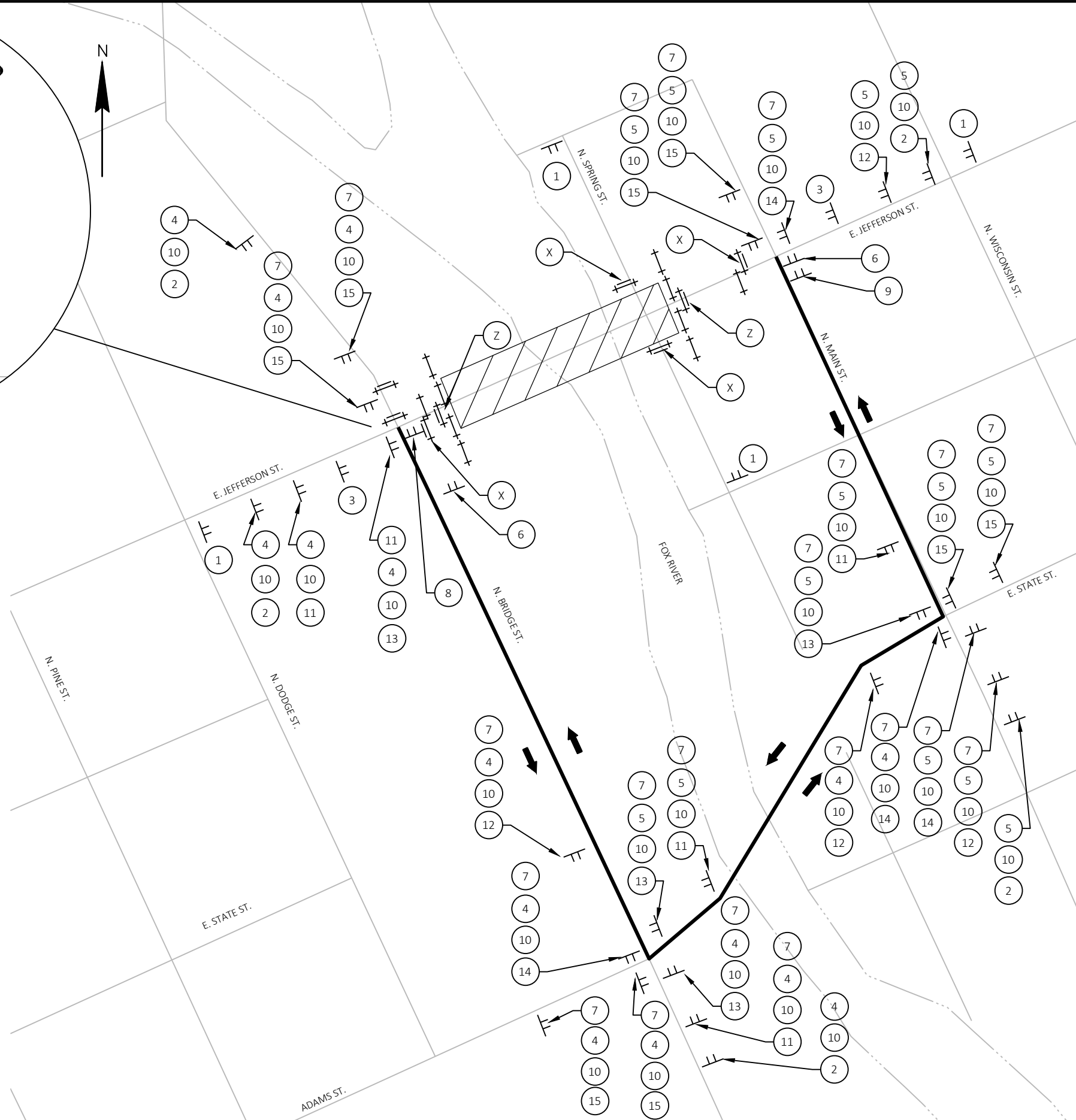
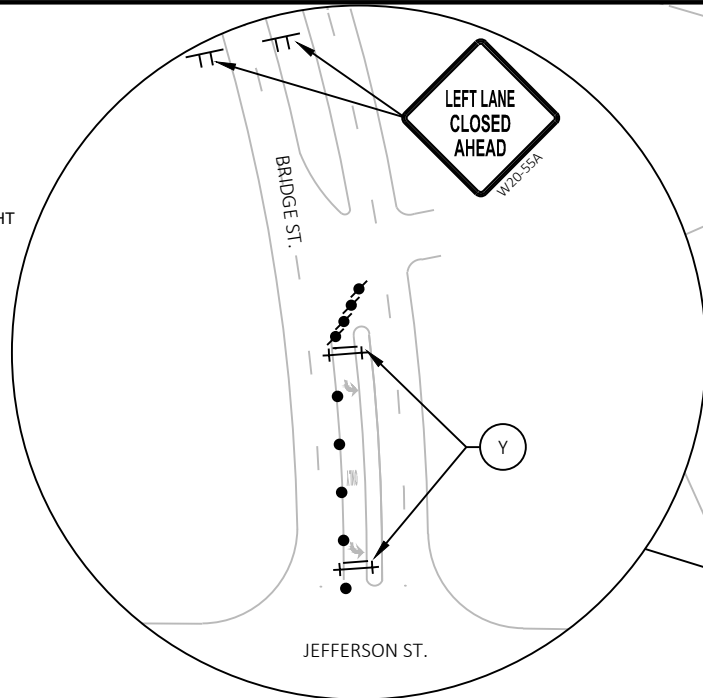
NO SCALE

POLE WIRING NOTES:



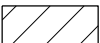

- ① POLE HAND HOLE AND COVER
- ② INSULATED SPLICE
- ③ UNGROUNDED (CIRCUIT) CONDUCTOR
- ④ #6 CONDUCTOR TO GROUND ROD
- ⑤ SYSTEM GROUNDING CONDUCTORS
- ⑥ BOND JUNCTION BOX
- ⑦ GROUNDING LUG
- ⑧ INLINE 1-POLE OR 2-POLE FUSE ASSEMBLY AND 5AMP FAST ACTING FUSES. WEATHERPROOF BOOTS.
- ⑨ 3-12 AWG XHHW (XLP) TO LUMINAIRE OR RECEPTACLE
- ⑩ 12" PIGTAIL BETWEEN SPLICE AND FUSE ASSEMBLY
- ⑪ CIRCUIT TAG
- ⑫ REVERSIBLE PRESSURE OR COMPRESSION GROUNDING CONNECTION (NOT INSULATED)
- ⑬ GROUNDED (NEUTRAL) CONDUCTOR
- ⑭ BRIDGE JUNCTION BOX
- ⑮ 5-12 AWG XHHW (XLP)

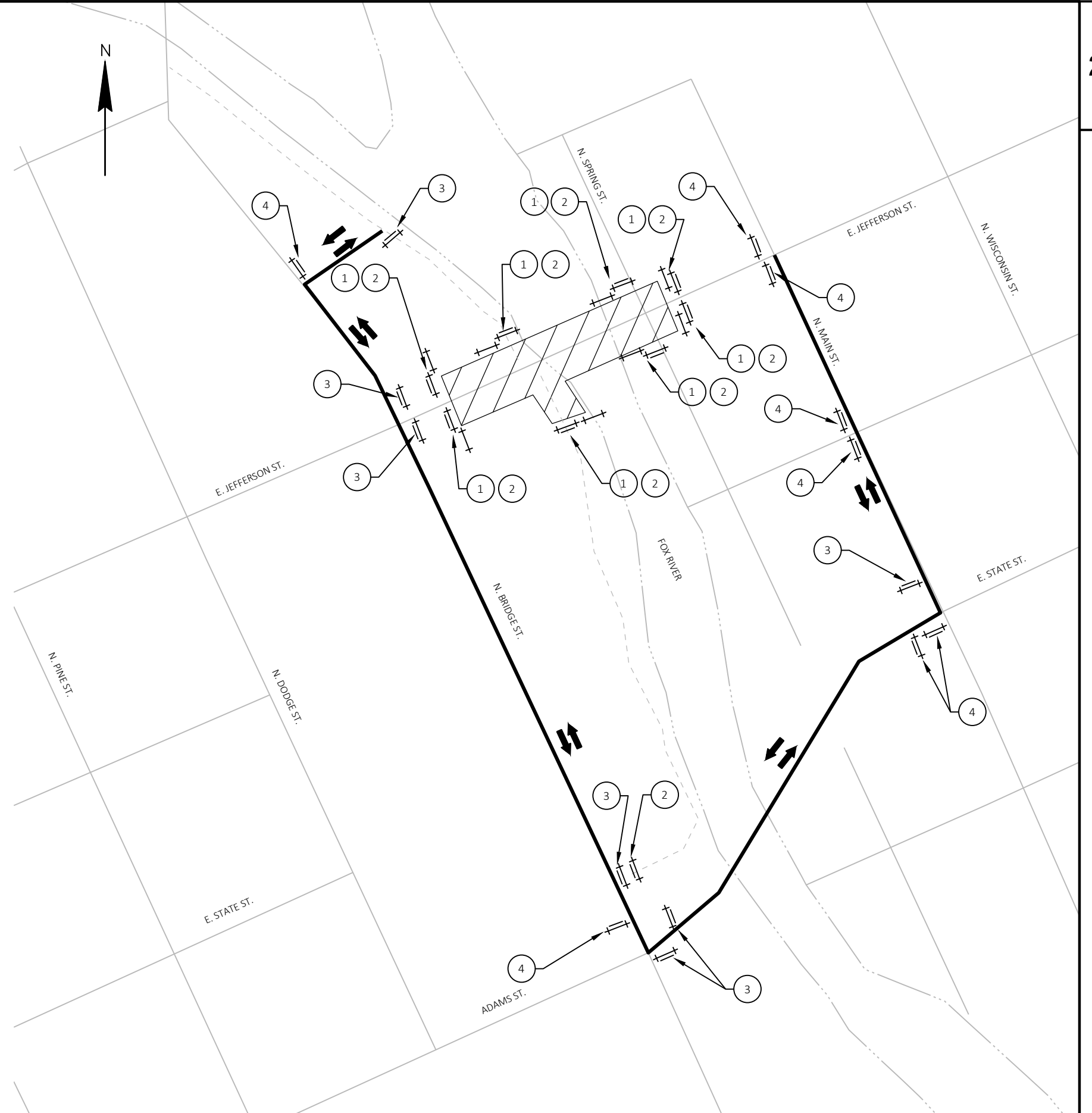
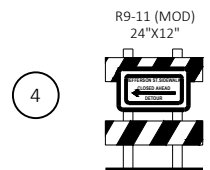
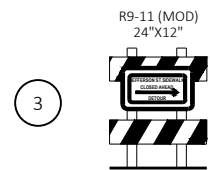
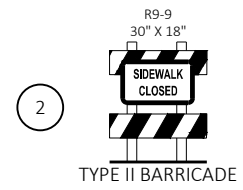
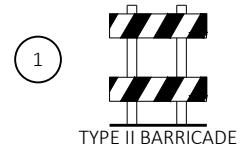
LEGEND

- ↑ TYPE III BARRICADE
- ↑↓ TYPE III BARRICADE WITH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
- ▬ SIGN ON PERMANENT SUPPORT
- ▬ SIGN ON TEMPORARY SUPPORT
- ⊙ TYPE A WARNING LIGHT (FLASHING)
- ⊙ TYPE C WARNING LIGHT (STEADY BURN)
- ▨ WORK AREA
- ➔ DIRECTION OF TRAFFIC

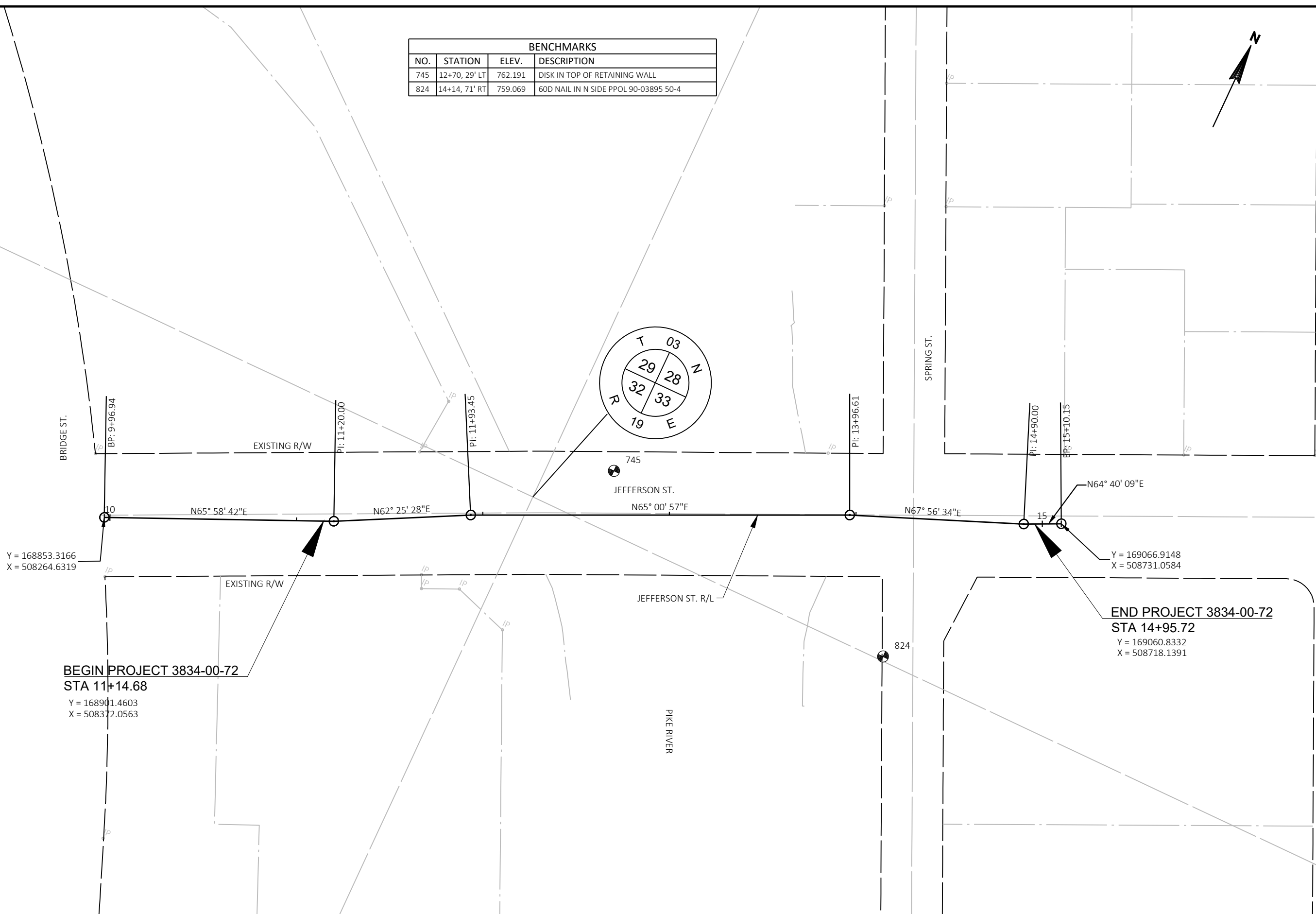
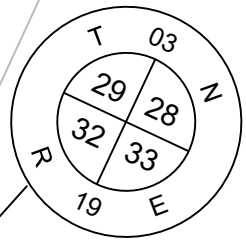




- LEGEND**
-  TYPE II BARRICADE
 -  TYPE II BARRICADE WITH ATTACHED SIGN
 -  WORK AREA
 -  DIRECTION OF TRAFFIC



BENCHMARKS			
NO.	STATION	ELEV.	DESCRIPTION
745	12+70, 29' LT	762.191	DISK IN TOP OF RETAINING WALL
824	14+14, 71' RT	759.069	60D NAIL IN N SIDE PPOL 90-03895 50-4



Y = 168853.3166
X = 508264.6319

BEGIN PROJECT 3834-00-72
STA 11+14.68
Y = 168901.4603
X = 508372.0563

Y = 169066.9148
X = 508731.0584

END PROJECT 3834-00-72
STA 14+95.72
Y = 169060.8332
X = 508718.1391

Estimate Of Quantities

3834-00-72

Line	Item	Item Description	Unit	Total	Qty
0002	203.0260	Removing Structure Over Waterway Minimal Debris (structure) 01. B-51-154	EACH	1.000	1.000
0004	204.0100	Removing Concrete Pavement	SY	887.000	887.000
0006	204.0130	Removing Curb	LF	6.000	6.000
0008	204.0150	Removing Curb & Gutter	LF	364.000	364.000
0010	204.0155	Removing Concrete Sidewalk	SY	245.000	245.000
0012	204.0220	Removing Inlets	EACH	2.000	2.000
0014	204.0245	Removing Storm Sewer (size) 01. 12-Inch	LF	31.000	31.000
0016	205.0100	Excavation Common	CY	494.000	494.000
0018	206.1000	Excavation for Structures Bridges (structure) 01. B-51-0154	LS	1.000	1.000
0020	206.5001	Cofferdams (structure) 01. B-51-154	EACH	2.000	2.000
0022	210.1500	Backfill Structure Type A	TON	630.000	630.000
0024	213.0100	Finishing Roadway (project) 01. 3834-00-72	EACH	1.000	1.000
0026	305.0110	Base Aggregate Dense 3/4-Inch	TON	4.000	4.000
0028	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	1,032.000	1,032.000
0030	415.0410	Concrete Pavement Approach Slab	SY	107.000	107.000
0032	416.0160	Concrete Driveway 6-Inch	SY	11.000	11.000
0034	455.0605	Tack Coat	GAL	91.000	91.000
0036	465.0105	Asphaltic Surface	TON	225.000	225.000
0038	465.0120	Asphaltic Surface Driveways and Field Entrances	TON	2.000	2.000
0040	465.0315	Asphaltic Flumes	SY	9.000	9.000
0042	502.0100	Concrete Masonry Bridges	CY	1,228.000	1,228.000
0044	502.3200	Protective Surface Treatment	SY	1,010.000	1,010.000
0046	502.3210	Pigmented Surface Sealer	SY	55.000	55.000
0048	502.9000.S	Underwater Substructure Inspection (structure) 01. B-51-154	EACH	2.000	2.000
0050	505.0400	Bar Steel Reinforcement HS Structures	LB	14,810.000	14,810.000
0052	505.0600	Bar Steel Reinforcement HS Coated Structures	LB	137,490.000	137,490.000
0054	505.0800.S	Bar Steel Reinforcement HS Stainless Structures	LB	450.000	450.000
0056	511.1200	Temporary Shoring (structure) 01. B-51-154	SF	600.000	600.000
0058	513.7016	Railing Steel Type C3	LF	412.000	412.000
0060	513.8016	Railing Steel Pedestrian Type C3 01. B-51-154	LF	29.000	29.000
0062	516.0500	Rubberized Membrane Waterproofing	SY	24.000	24.000
0064	517.1010.S	Concrete Staining (structure) 01. B-51-154	SF	3,080.000	3,080.000
0066	517.1015.S	Concrete Staining Multi-Color (structure) 01. B-51-154	SF	3,050.000	3,050.000
0068	517.1050.S	Architectural Surface Treatment (structure) 01. B-51-154	SF	3,050.000	3,050.000
0070	531.8990	Anchor Assemblies Poles on Structures	EACH	2.000	2.000
0072	550.0010	Pre-Boring Unconsolidated Materials	LF	1,051.000	1,051.000
0074	550.0020	Pre-Boring Rock or Consolidated Materials	LF	365.000	365.000
0076	550.1120	Piling Steel HP 12-Inch X 53 Lb	LF	1,980.000	1,980.000
0078	601.0110	Concrete Curb Type D	LF	6.000	6.000
0080	601.0411	Concrete Curb & Gutter 30-Inch Type D	LF	134.000	134.000
0082	602.0405	Concrete Sidewalk 4-Inch	SF	814.000	814.000
0084	602.0415	Concrete Sidewalk 6-Inch	SF	1,388.000	1,388.000
0086	602.0505	Curb Ramp Detectable Warning Field Yellow	SF	50.000	50.000
0088	602.0605	Curb Ramp Detectable Warning Field Radial Yellow	SF	17.000	17.000
0090	606.0300	Riprap Heavy	CY	120.000	120.000
0092	608.0512	Storm Sewer Pipe Reinforced Concrete Class V 12-Inch	LF	58.000	58.000
0094	611.0639	Inlet Covers Type H-S	EACH	2.000	2.000
0096	611.1230	Catch Basins 2x3-FT	EACH	2.000	2.000
0098	611.8115	Adjusting Inlet Covers	EACH	2.000	2.000

Estimate Of Quantities

3834-00-72

Line	Item	Item Description	Unit	Total	Qty
0100	612.0406	Pipe Underdrain Wrapped 6-Inch	LF	250.000	250.000
0102	618.0100	Maintenance And Repair of Haul Roads (project) 01. 3834-00-72	EACH	1.000	1.000
0104	619.1000	Mobilization	EACH	1.000	1.000
0106	624.0100	Water	MGAL	10.000	10.000
0108	625.0100	Topsoil	SY	309.000	309.000
0110	627.0200	Mulching	SY	14.000	14.000
0112	628.1504	Silt Fence	LF	63.000	63.000
0114	628.1520	Silt Fence Maintenance	LF	63.000	63.000
0116	628.1905	Mobilizations Erosion Control	EACH	3.000	3.000
0118	628.1910	Mobilizations Emergency Erosion Control	EACH	3.000	3.000
0120	628.2008	Erosion Mat Urban Class I Type B	SY	211.000	211.000
0122	628.6005	Turbidity Barriers	SY	374.000	374.000
0124	628.7020	Inlet Protection Type D	EACH	6.000	6.000
0126	628.7570	Rock Bags	EACH	15.000	15.000
0128	629.0210	Fertilizer Type B	CWT	2.000	2.000
0130	630.0140	Seeding Mixture No. 40	LB	2.000	2.000
0132	630.0500	Seed Water	MGAL	2.000	2.000
0134	631.0300	Sod Water	MGAL	3.000	3.000
0136	631.1000	Sod Lawn	SY	98.000	98.000
0138	634.0614	Posts Wood 4x6-Inch X 14-FT	EACH	2.000	2.000
0140	634.0810	Posts Tubular Steel 2x2-Inch X 10-FT	EACH	1.000	1.000
0142	634.0812	Posts Tubular Steel 2x2-Inch X 12-FT	EACH	2.000	2.000
0144	637.2210	Signs Type II Reflective H	SF	27.250	27.250
0146	637.2230	Signs Type II Reflective F	SF	16.500	16.500
0148	638.2102	Moving Signs Type II	EACH	1.000	1.000
0150	638.2602	Removing Signs Type II	EACH	10.000	10.000
0152	638.3000	Removing Small Sign Supports	EACH	5.000	5.000
0154	642.5001	Field Office Type B	EACH	1.000	1.000
0156	643.0300	Traffic Control Drums	DAY	569.000	569.000
0158	643.0410	Traffic Control Barricades Type II	DAY	2,600.000	2,600.000
0160	643.0420	Traffic Control Barricades Type III	DAY	731.000	731.000
0162	643.0705	Traffic Control Warning Lights Type A	DAY	1,463.000	1,463.000
0164	643.0900	Traffic Control Signs	DAY	13,085.000	13,085.000
0166	643.1050	Traffic Control Signs PCMS	DAY	35.000	35.000
0168	643.5000	Traffic Control	EACH	1.000	1.000
0170	645.0111	Geotextile Type DF Schedule A	SY	180.000	180.000
0172	645.0120	Geotextile Type HR	SY	260.000	260.000
0174	646.1020	Marking Line Epoxy 4-Inch	LF	756.000	756.000
0176	646.6120	Marking Stop Line Epoxy 18-Inch	LF	18.000	18.000
0178	646.7520	Marking Crosswalk Epoxy Block Style 24-Inch	LF	34.000	34.000
0180	650.4000	Construction Staking Storm Sewer	EACH	2.000	2.000
0182	650.5000	Construction Staking Base	LF	204.000	204.000
0184	650.5500	Construction Staking Curb Gutter and Curb & Gutter	LF	370.000	370.000
0186	650.6501	Construction Staking Structure Layout (structure) 01. B-51-154	EACH	1.000	1.000
0188	650.7000	Construction Staking Concrete Pavement	LF	30.000	30.000
0190	650.8501	Construction Staking Electrical Installations (project) 01. 3834-00-72	EACH	1.000	1.000
0192	650.9000	Construction Staking Curb Ramps	EACH	6.000	6.000
0194	650.9500	Construction Staking Sidewalk (project) 01. 3834-00-72	EACH	1.000	1.000
0196	650.9911	Construction Staking Supplemental Control (project) 01. 3834-0072	EACH	1.000	1.000

Estimate Of Quantities

3834-00-72

Line	Item	Item Description	Unit	Total	Qty
0198	650.9920	Construction Staking Slope Stakes	LF	204.000	204.000
0200	652.0125	Conduit Rigid Metallic 2-Inch	LF	50.000	50.000
0202	652.0225	Conduit Rigid Nonmetallic Schedule 40 2-Inch	LF	1,070.000	1,070.000
0204	652.0605	Conduit Special 2-Inch	LF	200.000	200.000
0206	653.0222	Junction Boxes 18x12x6-Inch	EACH	2.000	2.000
0208	655.0620	Electrical Wire Lighting 8 AWG	LF	4,323.000	4,323.000
0210	690.0150	Sawing Asphalt	LF	23.000	23.000
0212	690.0250	Sawing Concrete	LF	225.000	225.000
0214	715.0502	Incentive Strength Concrete Structures	DOL	7,350.000	7,350.000
0216	715.0720	Incentive Compressive Strength Concrete Pavement	DOL	500.000	500.000
0218	999.1001.S	Seismograph	EACH	1.000	1.000
0220	999.1501.S	Crack and Damage Survey	EACH	1.000	1.000
0222	999.2000.S	Installing and Maintaining Bird Deterrent System (station) 01. 13+00	EACH	1.000	1.000
0224	ASP.1T0A	On-the-Job Training Apprentice at \$5.00/HR	HRS	1,600.000	1,600.000
0226	ASP.1T0G	On-the-Job Training Graduate at \$5.00/HR	HRS	3,600.000	3,600.000
0228	SPV.0060	Special 01. Lighting Pull Boxes	EACH	1.000	1.000
0230	SPV.0060	Special 02. Decorative Lighting Unit Concrete Bases	EACH	1.000	1.000
0232	SPV.0060	Special 03. Decorative Lighting Units	EACH	3.000	3.000
0234	SPV.0060	Special 04. Utility Line Opening (ULO)	EACH	1.000	1.000
0236	SPV.0060	Special 05. Building Electrical Work	EACH	1.000	1.000
0238	SPV.0090	Special 01. Concrete Curb & Gutter 24-Inch, Type A	LF	60.000	60.000
0240	SPV.0090	Special 02. Concrete Curb & Gutter 24-Inch, Type D	LF	170.000	170.000
0242	SPV.0090	Special 04. NW Wingwall Decorative Railing	LF	27.000	27.000

PAVEMENT REMOVALS

CATEGORY	ROADWAY	STATION - STATION	LOCATION	204.0100 REMOVING CONCRETE PAVEMENT SY
0010	JEFFERSON STREET	11+15 - 11+79	Mainline	312
		14+11 - 14+95	Mainline	575
TOTALS				887

REMOVING CONCRETE ITEMS

CATEGORY	ROADWAY	STATION - STATION	LOCATION	204.0130 REMOVING CURB LF	204.0150 REMOVING CURB & GUTTER LF	204.0155 REMOVING CONCRETE SIDEWALK SY
0010	JEFFERSON STREET	11+15 - 11+94	LT	6	85	82
		11+15 - 11+94	RT	--	79	73
		13+96 - 14+95	LT	--	97	56
		13+96 - 14+95	RT	--	103	35
TOTALS				6	364	245

REMOVING/ADJUSTING STORM SEWER

CATEGORY	ROADWAY	STATION	LOCATION	204.0220 REMOVING INLETS EACH	204.0245 REMOVING STORM SEWER 0.1. 12-INCH LF	611.8115 ADJUSTING INLET COVERS EACH
0010	JEFFERSON STREET	11+99	RT	1	--	--
		12+00	LT	1	--	--
		12+00	LT/RT	--	31	--
		14+00	LT/RT	--	--	2
TOTALS				2	31	2

Division	From/To Station	Location	Common Excavation (1) Item 205.0100	Salvaged/Unusable Pavement Material (4)	Available Material (5)	Unexpanded Fill	Expanded Fill (7)	Mass Ordinate +/- (8)	Waste
			Cut (2)				Factor 1.25		
Jefferson Street	11+15 - 14+96		494	91	403	2	3	410	410
Grand Total			494	91	403	2	3	410	410

1) Common Excavation is the sum of the Cut and EBS Excavation columns. Item number 205.0100

2) Salvaged/Unusable Pavement Material is included in Cut.

3) No EBS Excavation to be backfilled.

4) Salvaged/Unusable Pavement Material

5) Available Material = Cut - Salvaged/Unusable Pavement Material

6) No Reduced EBS in Fill.

7) Expanded Fill. Factor = 1.25

Expanded Fill = (Unexpanded Fill - Reduced EBS) * Fill Factor

8) The Mass Ordinate + or - Qty calculated for the Division. Plus quantity indicates an excess of material within the Division. Minus indicates a shortage of material within the Division.

3

3

BASE AGGREGATE DENSE

CATEGORY	ROADWAY	STATION - STATION	LOCATION	305.0110	305.0120	624.0100
				BASE AGGREGATE DENSE 3/4-INCH TON	BASE AGGREGATE DENSE 1 1/4-INCH TON	WATER MGAL
0010	JEFFERSON STREET					
		11+15 - 11+79	MAINLINE	--	181	2
		11+79 - 11+94	WEST APPROACH SLAB	--	21	1
		13+96 - 14+11	EAST APPROACH SLAB	--	20	1
		14+11 - 14+95	MAINLINE	--	367	4
		11+15 - 14+95	CURB AND GUTTER, LT	--	55	1
		11+15 - 14+95	CURB AND GUTTER, RT	--	55	1
		11+15 - 14+95	SHOULDER RT	4	17	1
		11+15 - 14+95	DRIVEWAY, LT	--	5	--
TOTALS				4	722	10

CONCRETE ITEMS

CATEGORY	ROADWAY	STATION - STATION	LOCATION	415.0410	416.0160	601.0110	601.0411	602.0405	602.0415	SPV.0090.01	SPV.0090.02
				CONCRETE PAVEMENT APPROACH SLAB SY	CONCRETE DRIVEWAY 6-INCH SY	CONCRETE CURB TYPE D LF	CONCRETE CURB AND GUTTER 30-INCH TYPE D LF	CONCRETE SIDEWALK 4-INCH SF	CONCRETE SIDEWALK 6-INCH SF	CONCRETE CURB AND GUTTER 24-INCH, TYPE A LF	CONCRETE CURB AND GUTTER 24-INCH, TYPE D LF
0010	JEFFERSON STREET										
		11+15 - 11+94	LT	27	--	6	--	--	735	15	70
		11+15 - 11+94	RT	27	11	--	--	--	653	15	64
		13+96 - 14+95	LT	27	--	--	63	501	--	15	19
		13+96 - 14+95	RT	26	--	--	71	313	--	15	17
TOTALS				107	11	6	134	814	1,388	60	170

ASPHALTIC ITEMS

CATEGORY	ROADWAY	STATION - STATION	LOCATION	455.0605	465.0105	465.0120	465.0315
				TACK COAT GAL	ASPHLATIC SURFACE TON	ASPHALTIC SURFACE DRIVEWAY & FIELD ENTRANCE TON	ASPHALTIC FLUMES SY
0010	JEFFERSON STREET						
		11+15 - 11+79	Mainline	36	75	2	--
		14+11 - 14+95	Mainline	55	150	--	9
TOTALS				91	225	2	9

CURB RAMP ITEMS

CATEGORY	ROADWAY	STATION - STATION	LOCATION	602.0505	602.0605
				CURB RAMP DETECTABLE WARNING FIELD YELLOW SF	CURB RAMP DETECTABLE WARNING FIELD RADIAL YELLOW SF
0010	JEFFERSON STREET				
		11+15 - 11+94	LT	10	--
		11+15 - 11+94	RT	10	--
		13+96 - 14+95	LT	20	--
		13+96 - 14+95	RT	10	17
TOTALS				50	17

STORM SEWER

CATEGORY	ROADWAY	STRUCTURE NUMBER	STATION	O/S	COVER ELEV	608.0512	611.0639	611.1230	DEPTH FT	FROM STRUCTURE	TO STRUCTURE	INLET ELEV	DISCH ELEV	PIPE TIES REQ'D EACH	
						REINFORCED CONCRETE CLASS V 12-INCH LF	INLET COVERS TYPE H-S EACH	CATCH BASINS 2X3 EACH							
0010	JEFFERSON STREET														
		1.1	11+89	17' LT	760.59	34	1	1	0.33%	5.70	1.1	1.2	754.48	756.15	--
		1.2	11+89	17' RT	760.60	24	1	1	1.25%	5.86	1.2	-	754.33	756.00	3
TOTALS						58	2	2							

** STRUCTURE DEPTH DOES NOT INCLUDE 6 INCHES OF ADJUSTMENT RINGS

* FOR INFORMATION ONLY

PROJECT NO: 3834-00-72

HWY: JEFFERSON STREET

COUNTY: RACINE

MISCELLANEOUS QUANTITIES

SHEET

E

3

3

LANDSCAPING ITEMS

CATEGORY	ROADWAY	STATION	-	STATION	LOCATION	625.0100 TOPSOIL SY	629.0210 FERTILIZER TYPE B CWT	630.0140 SEEDING MIXTURE NO. 40 LB	630.0500 SEED WATER MGAL	631.0300 SOD WATER MGAL	631.1000 SOD LAWN SY
0010	JEFFERSON STREET	11+15	-	11+94	LT	15	0.01	--	--	0.40	15
		11+15	-	11+94	RT	35	0.02	--	--	0.90	35
		13+96	-	14+95	LT	28	0.02	--	--	0.70	28
		13+96	-	14+95	RT	169	0.11	1.00	1.00	--	--
		SUBTOTAL				247	1	1	1	2	78
		UNDISTRIBUTED				62	1	1	1	1	20
TOTALS						309	2	2	2	3	98

EROSION CONTROL

CATEGORY	ROADWAY	STATION	TO	STATION	O/S	627.0200 MULCHING SY	628.1504 SILT FENCE LF	628.1520 SILT FENCE MAINTENANCE LF	628.1905 EROSION CONTROL EA	628.1910 EMERGENCY EROSION CONTROL EA	628.2008 MAT URBAN CLASS I SY	628.6005 TURBIDITY BARRIERS SY	628.7020 INLET PROTECTION TYPE D EA	628.7570 ROCK BAGS EA
0010	JEFFERSON STREET	11+15	-	11+94	LT/RT	2	--	--	1	1	--	160	4	--
		13+96	-	14+95	LT/RT	9	50	50	1	1	169	139	2	12
		SUBTOTAL				11	50	50	2	2	169	299	6	12
		UNDISTRIBUTED				3	13	13	1	1	42	75	--	3
TOTALS						14	63	63	3	3	211	374	6	15

PERMANENT SIGNING

STATION	LOCATION	SIGN NO.	SIGN CODE	W x H IN x IN	634.0614 SIGN SIZE EA	634.0810 POSTS WOOD EA	634.0812 TUBULAR STEEL EA	637.2210 POSTS TUBULAR STEEL EA	637.2230 SIGNS TYPE II REFLECTIVE H SF	638.2102 SIGNS TYPE II REFLECTIVE F SF	638.2602 MOVE SIGN TYPE II EA	638.3000 REMOVING SMALL SIGN SUPPORTS EA	DESCRIPTION	COMMENTS
JEFFERSON STREET														
11+32	32' LT	1-1	R3-2	24 x 24	-	1	-	4.00	-	-	1	1	REGULATORY SIGN	NO LEFT TURN SYMBOL
11+73	28' RT	1-2	W11-2 S16-7	30 x 30 24 x 12	-	-	1	-	6.25	-	1	1	WARNING SIGN	PEDESTRIAN CROSSING
11+93	26.5' LT	1-3	W11-2 S16-7	30 x 30 24 x 12	-	-	1	-	6.25	-	1	-	SCHOOL SIGN	DOWN ARROW SYMBOL
11+92	25' RT	1-4	I3-1	36 x 24	1	-	-	6.00	-	-	1	1	WARNING SIGN	PEDESTRIAN CROSSING
12+00	23.5' RT	1-5	R12-1	-	-	-	-	-	-	-	1	-	SCHOOL SIGN	DOWN ARROW SYMBOL
12+10	25.5' LT	1-6	R3-8	45 x 36	-	-	-	11.25	-	-	1	1	INFFORMATIONAL SIGN	FOX RIVER
13+98	26' LT	1-7	I3-1	36 x 24	1	-	-	6.00	-	-	1	1	REGULATORY SIGN	WEIGHT LIMIT 45 TONS
	23.5' LT	1-8	R12-1	-	-	-	-	-	-	-	1	-	REGULATORY SIGN	WEIGHT LIMIT 45 TONS
14+56	33' RT	1-9	R1-1	-	-	-	-	-	-	1	-	-	REGULATORY SIGN	STOP
TOTALS					2	1	2	27.25	16.5	1	10	5		

PROJECT NO: 3834-00-72

HWY: JEFFERSON STREET

COUNTY: RACINE

MISCELLANEOUS QUANTITIES

SHEET

E

3

3

TRAFFIC CONTROL ITEMS

CATEGORY	ROADWAY	LOCATION	643.0300	643.0410	643.0420	643.0705	643.0900	643.1050
			TRAFFIC CONTROL DRUMS	TRAFFIC CONTROL BARRICADES TYPE II	TRAFFIC CONTROL BARRICADES TYPE III	TRAFFIC CONTROL WARNING LIGHTS A	SIGNS	SIGNS PCMS
			DAYS	DAYS	DAYS	DAYS	DAYS	DAYS
0010	JEFFERSON STREET							
		BRIDGE STREET AND JEFFERSON STREET	455	780	585	1,170	2,795	14
		JEFFERSON STREET AND ADAMS STREET	--	325	--	--	2,600	--
		STATE STREET AND MAIN STREET	--	195	--	--	2,470	--
		MAIN STREET AND JEFFERSON STREET	--	780	--	--	2,600	14
		SUBTOTAL	455	2,080	585	1,170	10,465	28
		UNDISTRIBUTED	114	520	146	293	2,620	7
TOTALS			569	2,600	731	1,463	13,085	35

PAVEMENT MARKING

CATEGORY	ROADWAY	STATION TO	STATION	646.1020		646.6120	646.7520
				MARKING	MARKING	MARKING	
				LINE	STOP LINE	CROSSWALK EPOXY	
				EPOXY 4-INCH	EPOXY	BLOCK STYLE	
				YELLOW	WHITE	18-INCH	24-INCH
				LF	LF	LF	LF
0010	JEFFERSON STREET						
		11+15	- 14+95	694	62	18	34
TOTALS				756	18	34	

CONSTRUCTION STAKING ITEMS

CATEGORY	ROADWAY	STATION - STATION	LOCATION	650.4000	650.5000	650.5500	650.7000	650.8501	650.9000	650.9500	650.9911	650.9920
				CONSTRUCTION STAKING STORM SEWER	CONSTRUCTION STAKING BASE	CONSTRUCTION CURB GUTTER AND CURB & GUTTER	CONSTRUCTION STAKING CONCRETE PAVEMENT	CONSTRUCTION STAKING ELECTRICAL INSTALLATIONS	CONSTRUCTION STAKING CURB RAMPS	CONSTRUCTION STAKING SIDEWALK	CONSTRUCTION SUPPLEMENTAL CONTROL	CONSTRUCTION STAKING SLOPE STAKES
				EACH	LF	LF	LF	EACH	EACH	EACH	EACH	LF
0010	JEFFERSON STREET											
		11+15 - 12+15	MAINLINE	2	100	170	15	1	2	1	1	100
		13+92 - 14+96	MAINLINE	--	104	200	15	--	4	--	--	104
TOTALS				2	204	370	30	1	6	1	1	204

SAWING PAVEMENT ITEMS

CATEGORY	ROADWAY	STATION - STATION	LOCATION	690.0150	690.0151
				SAWING ASPHALT	SAWING CONCRETE
				LF	LF
0010	JEFFERSON STREET				
		11+15 - 14+95	MAINLINE	--	119
		11+15 - 14+95	SIDEWALK, LT	--	45
		11+15 - 14+95	SIDEWALK, RT	--	22
		11+15 - 14+95	DRIVEWAY, LT	23	4
		11+15 - 14+95	DRIVEWAY, RT	--	35
TOTALS				23	225

UTILITY LINE OPENING

CATEGORY	ROADWAY	STATION - STATION	LOCATION	SPV.0060.04
				UTILITY LINE OPENING (ULO)
				EACH
0010	JEFFERSON STREET			
		13+70 - 13+87	40', RT	1
TOTALS				1

LIGHTING PULL BOX QUANTITIES

NO.	SPV.0060.01 LIGHTING PULL BOXES EACH
LPB-1	1
TOTAL	1

BUILDING WORK QUANTITIES

LOC.	SPV.0060.05 BUILDING ELECTRICAL WORK EACH
BUILDING	1
TOTAL	1

LIGHTING UNIT QUANTITIES

NO.	SPV.0060.02 DECORATIVE LIGHTING UNIT CONCRETE BASES EACH	SPV.0060.03 DECORATIVE LIGHTING UNITS EACH
BL-11/13L-15R-1	1	1
BL-11/13L-15R-2		1
BL-11/13L-15R-3		1
TOTALS	1	3

LIGHTING BRANCH CIRCUIT WIRE AND CONDUIT QUANTITIES

FROM	TO	652.0225 CONDUIT RIGID NONMETALLIC SCHEDULE 40 2-INCH LF	652.0605 CONDUIT SPECIAL 2-INCH * LF	655.0620 ELECTRICAL WIRE LIGHTING 8 AWG LF
BUILDING	EPB			1068
EPB	LPB-1		200	1060
LPB-1	BRIDGE (BL-11/13L-15R-2)	68		615
LPB-1	BL-11/13L-15R-1	60		360
BL-11/13L-15R-1	BRIDGE (BL-11/13L-15R-3)	72		1220
	TOTALS	200	200	4323

* CONDUIT NONMETALLIC SCHEDULE 40

NOTES:
THIS EXHIBIT IS A GRAPHIC REPRESENTATION AND IS FOR REFERENCE PURPOSES ONLY.
REFER TO THE CONVEYANCE DOCUMENT FOR PARCEL RELATED DETAILS.

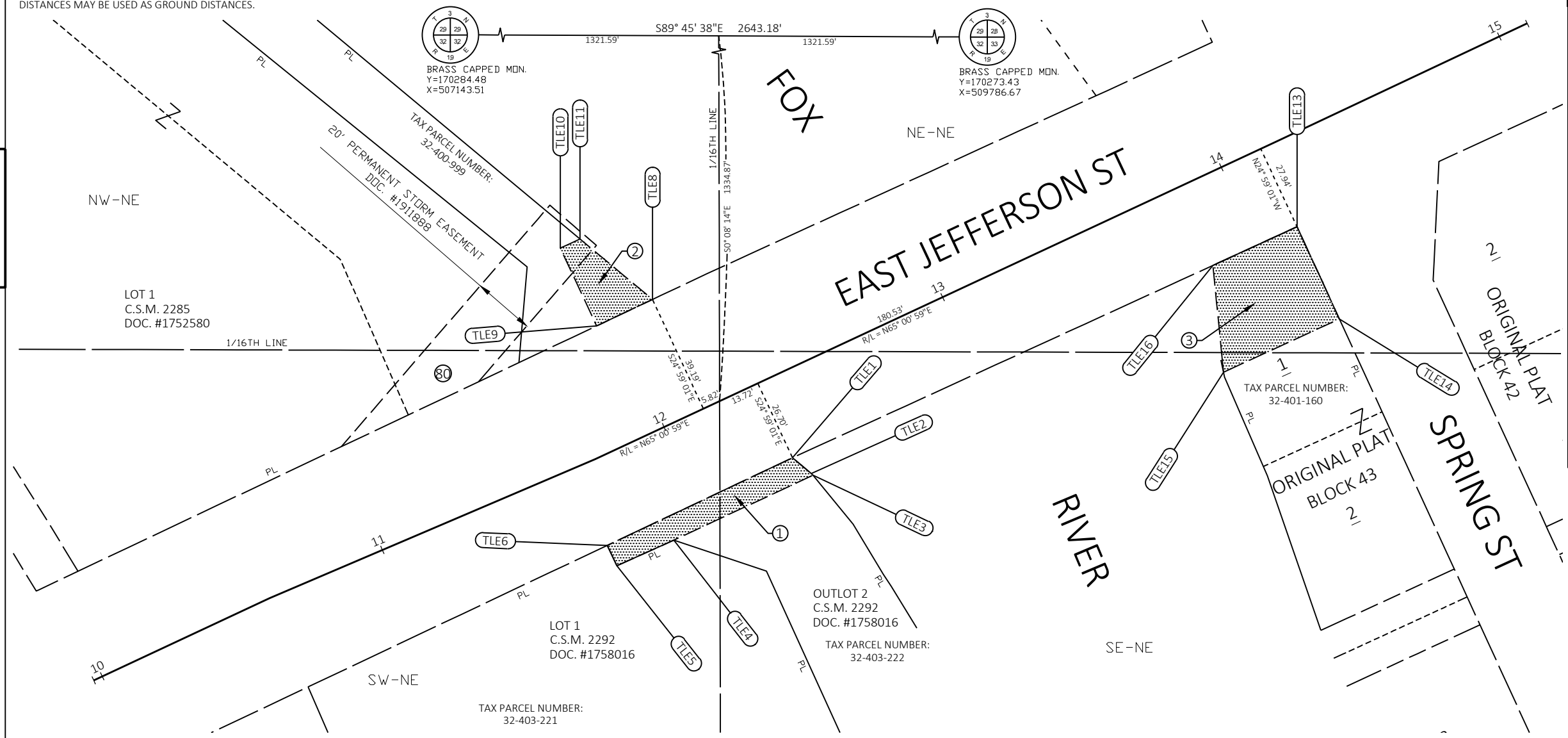
THE PURPOSE OF THE TLE IS FOR GRADING, UNLESS OTHERWISE NOTED.

POSITIONS SHOWN ON THIS EXHIBIT ARE WISCONSIN COUNTY COORDINATE SYSTEM (WCCS), RACINE COUNTY, NAD 83 (2011) IN US SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

R/W PROJECT NUMBER: 3834-00-02 SHEET NUMBER: 1
TLE ACQUISITION EXHIBIT
JEFFERSON ST
FOX RIVER BRIDGE B51-0001 RACINE COUNTY
PART OF THE NW 1/4 OF THE NE 1/4, NE 1/4 OF THE NE 1/4, SE 1/4 OF THE NE 1/4, AND SW 1/4 OF THE NE 1/4, ALL IN SECTION 32, T3N, R19E, CITY OF BURLINGTON, RACINE COUNTY, WISCONSIN.

STATION & OFFSET TABLE				
Point	Station	Offset	Northing	Easting
TLE 1	12+33.65	26.70'	168925.67	508492.00
TLE 2	12+37.07	33.94'	168920.55	508498.15
TLE 3	12+37.28	34.66'	168919.99	508498.65
TLE 4	11+87.33	34.66'	168898.89	508453.37
TLE 5	11+68.17	34.64'	168890.56	508434.85
TLE 6	11+67.90	27.33'	168897.18	508431.73
TLE 8	12+14.12	-39.19'	168977.15	508446.46
TLE 9	11+94.00	-39.08'	168968.55	508428.27
TLE 10	11+94.00	-67.00'	168993.86	508416.48
TLE 11	12+01.02	-67.00'	168996.82	508422.84
TLE 13	14+14.19	27.94'	169000.8	508656.16
TLE 14	14+14.02	60.94'	168970.81	508669.95
TLE 15	13+72.53	60.73'	168953.48	508632.25
TLE 16	13+84.03	27.79'	168988.20	508628.76

COURSE TABLE		
Point - Point	Bearing	Distance
TLE 1 - TLE 2	S50° 12' 54"E	8.01'
TLE 2 - TLE 3	S41° 27' 46"E	0.75'
TLE 3 - TLE 4	S65° 00' 59"W	49.95'
TLE 4 - TLE 5	S65° 47' 16"W	20.31'
TLE 5 - TLE 6	N25° 11' 40"W	7.32'
TLE 6 - TLE 1	N64° 41' 50"E	66.66'
TLE 8 - TLE 9	S64° 41' 50"W	20.12'
TLE 9 - TLE 10	N24° 59' 01"W	27.92'
TLE 10 - TLE 11	N65° 00' 59"E	7.02'
TLE 11 - TLE 8	S50° 12' 40"E	30.74'
TLE 13 - TLE 14	S24° 41' 14"E	33.00'
TLE 14 - TLE 15	S65° 18' 46"W	41.49'
TLE 15 - TLE 16	N05° 44' 21"W	34.89'
TLE 16 - TLE 13	N65° 18' 46"E	30.16'



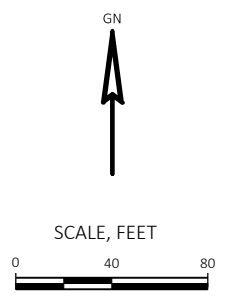
SCHEDULE OF LANDS & INTERESTS REQUIRED

PARCEL NUMBER	OWNER(S)	INTEREST REQUIRED	TLE S.F.
1	COMMUNITY DEVELOPMENT AUTHORITY OF THE CITY OF BURLINGTON	TLE	530
2	CITY OF BURLINGTON	TLE	378
3	CITY OF BURLINGTON	TLE	1182

UTILITY INTERESTS REQUIRED

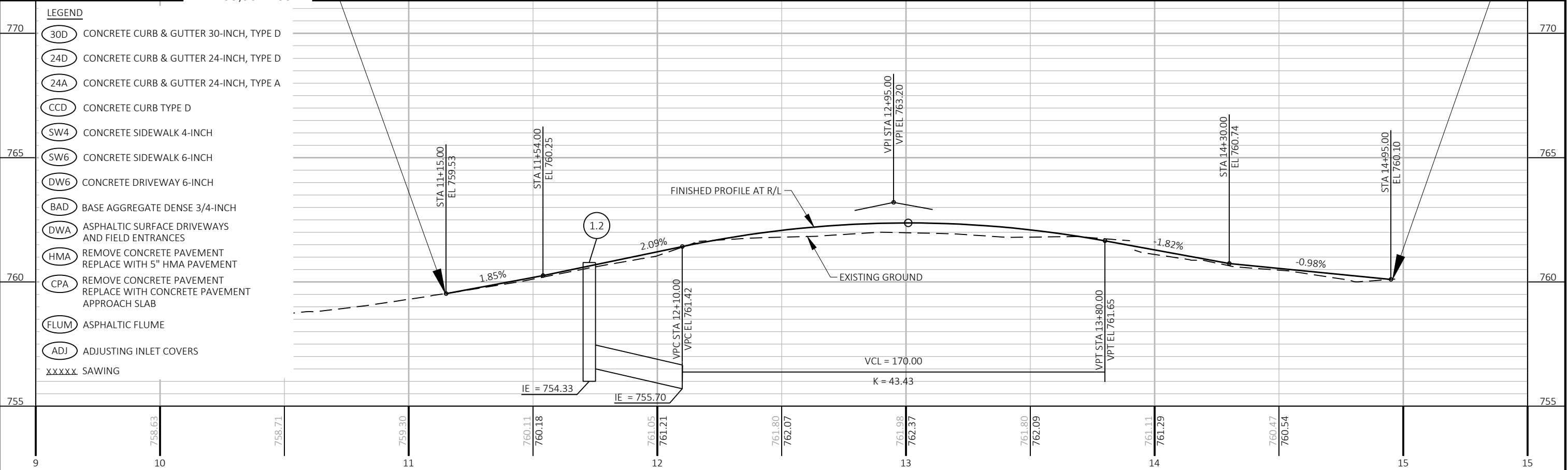
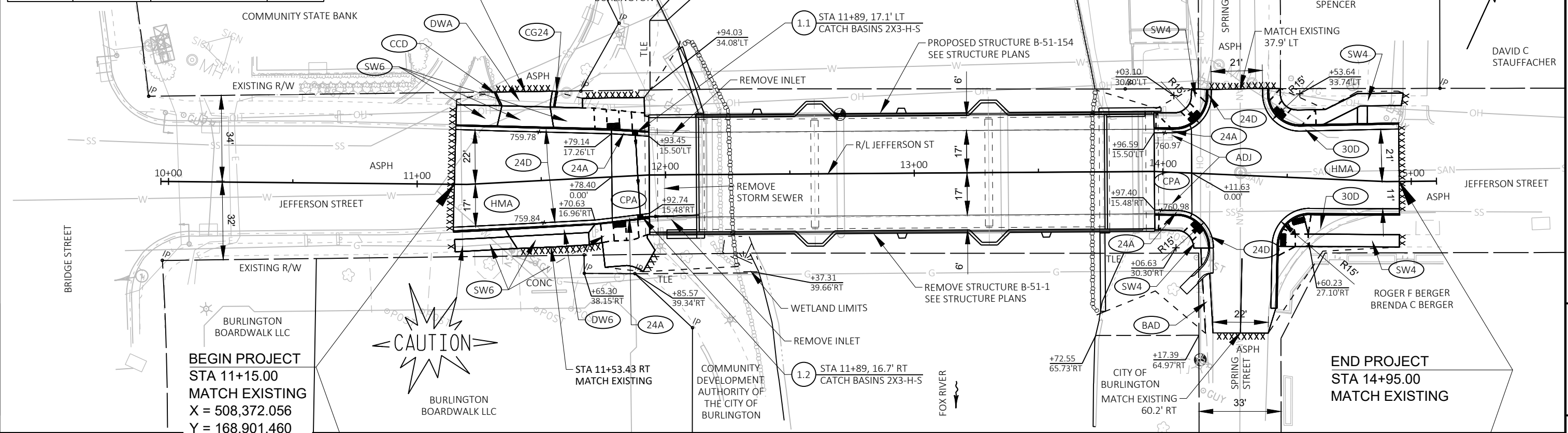
UTILITY NUMBER	UTILITY OWNER(S)	INTEREST REQUIRED
80	CITY OF BURLINGTON	TEMPORARY RELEASE OF RIGHTS

OWNERS NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE CITY OF BURLINGTON



THIS MAP IS APPROVED FOR THE CITY OF BURLINGTON
SIGNATURE: _____ DATE: _____
PRINT NAME: JEANNIE HEFTY

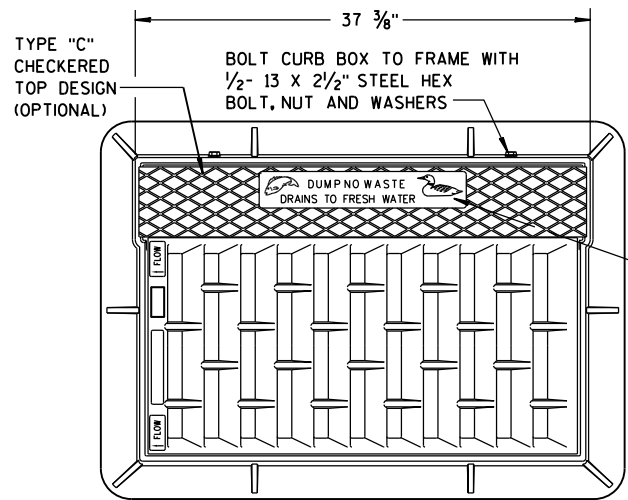
DRAINAGE TABLE			
STRUCTURE	COVER ELEV.	PIPE INFORMATION	IE
1.1	760.59	34' - 12-INCH @ 0.33%	756.15 (S)
1.2	760.60	24' = 12-INCH @ 1.25%	756.00 (E)



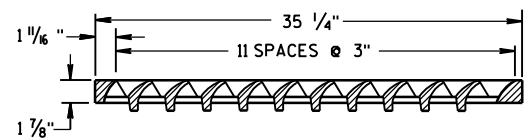
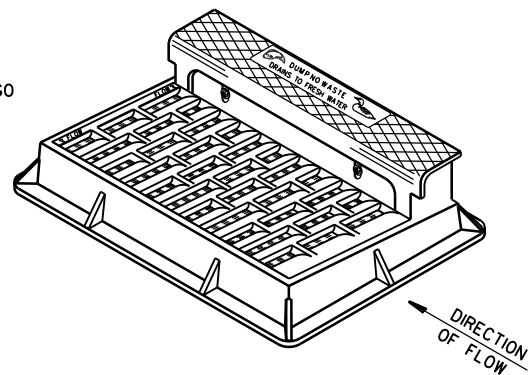
PROJECT NO: 3834-00-72	HWY: JEFFERSON STREET	COUNTY: RACINE	PLAN AND PROFILE: JEFFERSON STREET	SHEET	E
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Standard Detail Drawing List

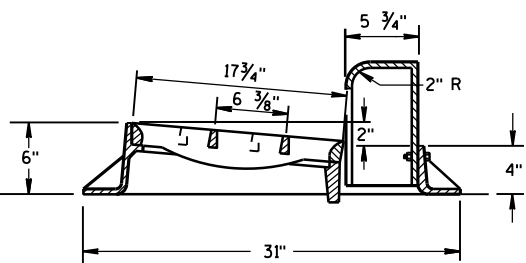
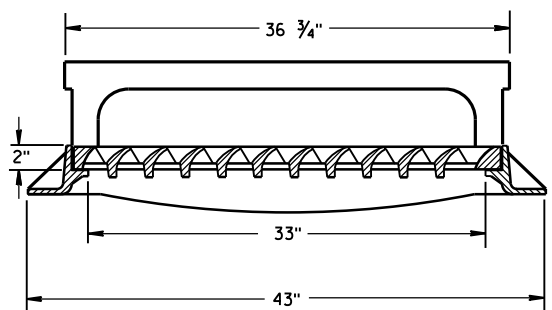
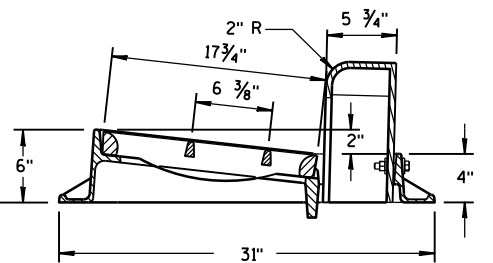
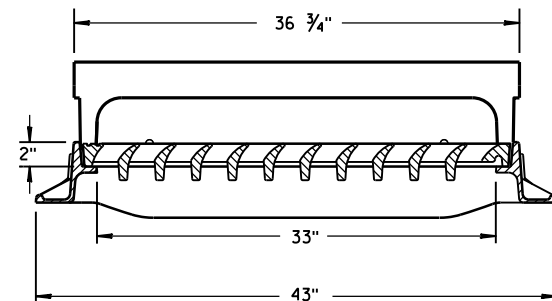
08A05-19A	INLET COVERS TYPE A, H, A-S, H-S & Z
08A09-02	CATCH BASINS 2X3-FT AND 2.5X3-FT
08D01-22A	CONCRETE CURB & GUTTER
08D01-22B	CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS
08D04-06	CONCRETE SURFACE DRAINS & ASPHALTIC FLUMES
08D05-20A	CURB RAMPS TYPES 1 AND 1-A
08D05-20B	CURB RAMPS TYPES 2 AND 3
08D05-20C	CURB RAMPS TYPES 4A AND 4A1
08D05-20D	CURB RAMPS TYPE 4B AND 4B1
08D05-20E	CURB RAMPS TYPES 5, 6, 7A, 7B & 8
08D05-20F	CURB RAMPS RADIAL DETECTABLE WARNING FIELD APPLICATIONS
08D05-20G	CURB RAMPS RECTANGULAR AND RADIAL DETECTABLE WARNING PLATES
08D19-03	DRIVEWAY AND SIDEWALK RAMPS TYPE Z
08E09-06	SILT FENCE
08E10-02	INLET PROTECTION TYPE A, B, C AND D
08E11-02	TURBIDITY BARRIER
09B02-10	CONDUIT
12A03-10	NAME PLATE (STRUCTURES)
13B02-09A	CONCRETE PAVEMENT APPROACH SLAB
13B02-09B	STRUCTURAL APPROACH SLAB AND CONCRETE PAVEMENT APPROACH SLAB
13C19-03	HMA LONGITUDINAL JOINTS
15C02-08A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-08B	BARRICADES AND SIGNS FOR VARIOUS CLOSURES
15C02-08C	DETOUR SIGNING FOR MAINLINE CLOSURES
15C03-05	BARRICADES AND SIGNS FOR SIDEROAD CLOSURES
15C08-21A	LONGITUDINAL MARKING (MAINLINE)
15C11-09B	CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS
15C33-04	STOP LINE AND CROSSWALK PAVEMENT MARKING
15C35-05A	PAVEMENT MARKING (INTERSECTIONS)



**NOTE:
GRATE IS REVERSIBLE.**

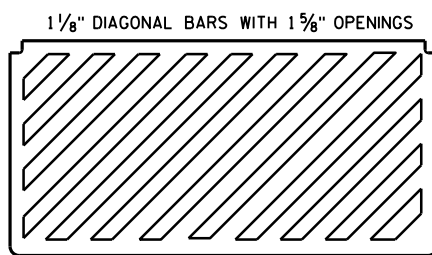


NOTE: CURB BOX HEIGHT ADJUSTABLE 6" TO 9"

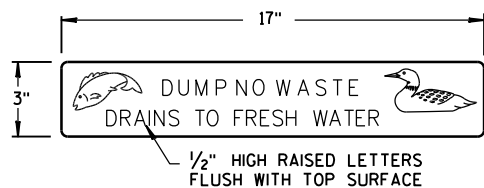


TYPE "H"

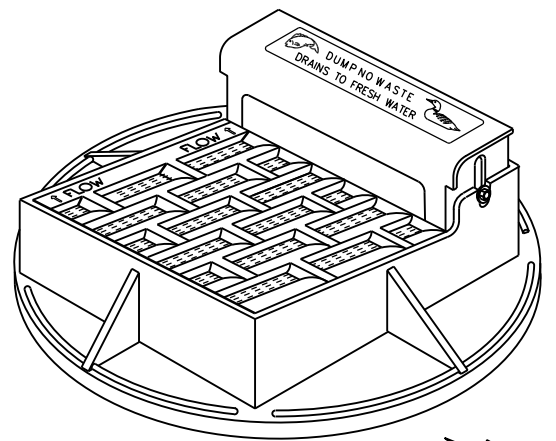
NOTE: EITHER CASTING IS ACCEPTABLE



**SPECIAL GRATE FOR
TYPE "H" COVER**
(MEASURES 35 1/4" X 17 3/4" X 2")
(NOTED AS TYPE H-S ON DRAINAGE TABLE)

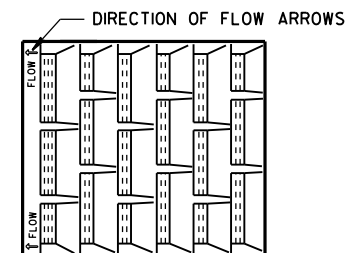


LOGO DETAIL

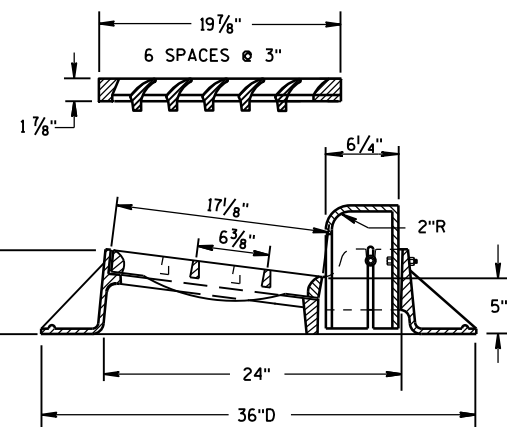
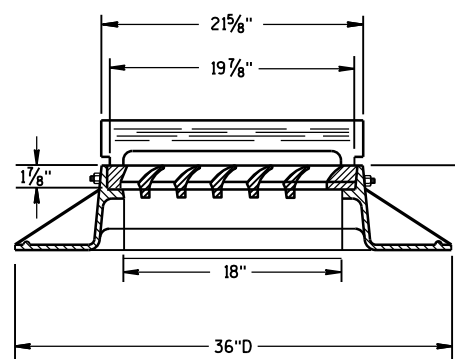


NOTE: CURB BOX ADJUSTABLE 4" TO 9"

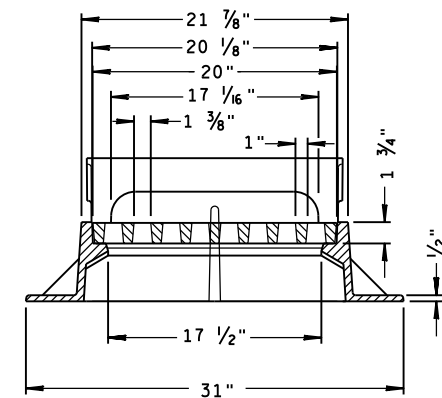
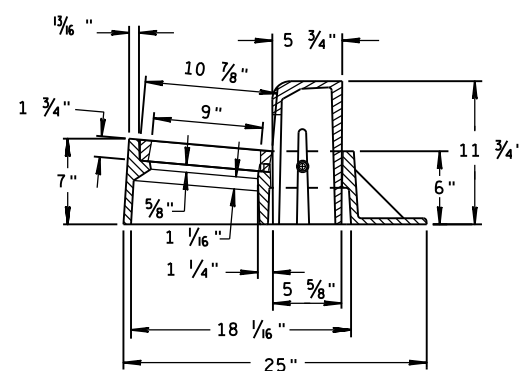
**NOTE:
GRATE IS REVERSIBLE.**



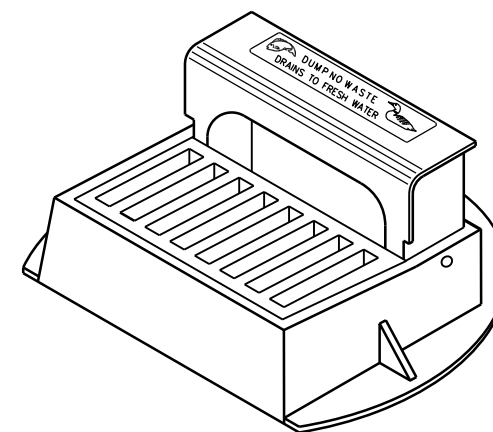
**SPECIAL GRATE FOR
TYPE "A" COVER**
(MEASURES 19 3/4" X 17" X 1 1/8")
(NOTED AS TYPE A-S ON DRAINAGE TABLE)



TYPE "A"



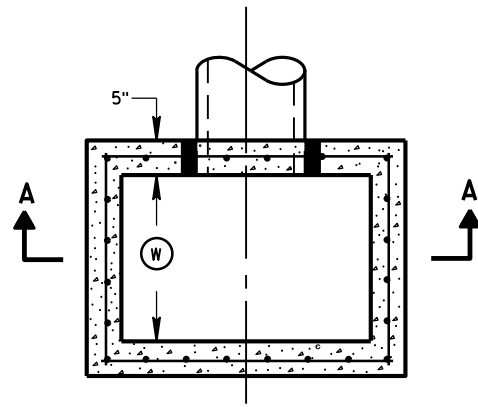
TYPE "Z"



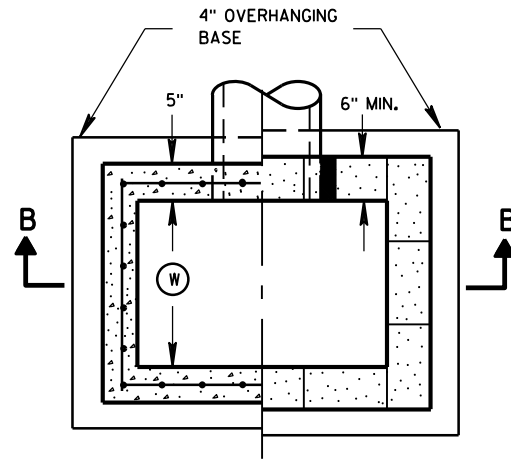
**INLET COVERS
TYPE A, H, A-S, H-S & Z**

**STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION**

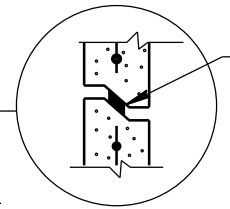
APPROVED
11-27-13
DATE
/S/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT
ENGINEER
FHWA



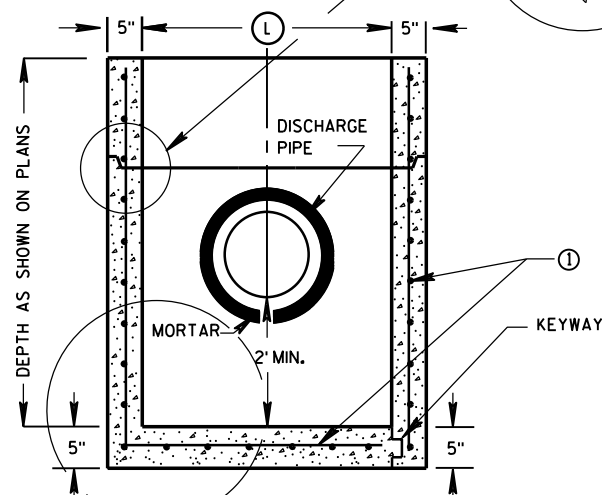
PLAN VIEW



PLAN VIEW

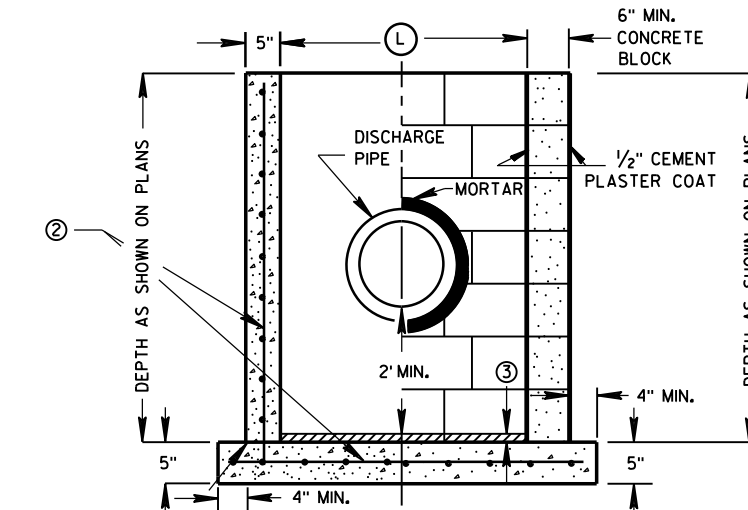


RISER JOINTS TO BE SEALED WITH A BUTYL RUBBER SEAL PER SEALANT MANUFACTURERS RECOMMENDATIONS CONFORMING TO ASTM C 990 (TYP)



PRECAST REINFORCED CONCRETE WITH MONOLITHIC BASE

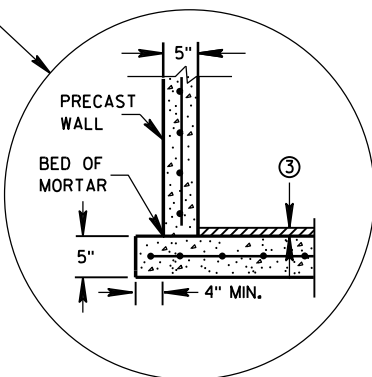
SECTION A-A



CAST-IN-PLACE REINFORCED CONCRETE

CONCRETE BLOCK ON CAST-IN-PLACE WITH PRECAST REINFORCED CONCRETE BASE ①

SECTION B-B



SEPARATE PRECAST REINFORCED CONCRETE BASE OPTION

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST CATCH BASIN UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL PRECAST CATCH BASIN UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF ASTM C 913.

ALL DRAINAGE STRUCTURES ARE DESIGNATED ON THE PLANS AS "MANHOLES 3X3-L", "CATCH BASINS 4-B", "INLETS 2X3-H", ETC. THE FIRST NUMBERS DESIGNATES THE SIZE OF THE STRUCTURE, AND THE FOLLOWING LETTER DESIGNATES THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF FOUNDATION BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

4" OVERHANGING BASES ARE REQUIRED FOR CAST-IN-PLACE REINFORCED CONCRETE AND CONCRETE BLOCK INSTALLATIONS. 4" OVERHANG IS REQUIRED WHEN SEPARATE PRECAST BASE IS PROVIDED. OVERHANG IS NOT REQUIRED ON PRECAST STRUCTURES WITH AN INTEGRAL OR MONOLITHIC BASE.

MAXIMUM INSIDE PIPE DIAMETER DETERMINED BY 3" CLEARANCE ON EACH SIDE OF THE OUTSIDE WALL OF THE PIPE. SEE DETAIL "A". ASSUMES PIPE ENTERS PERPENDICULAR TO THE STRUCTURE.

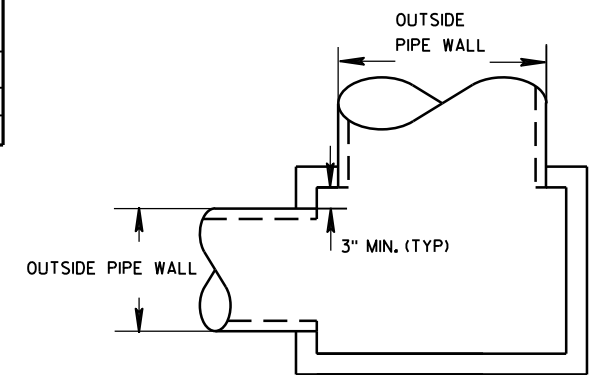
- ① FOR PRECAST CATCH BASINS PROVIDE REINFORCING STEEL IN ACCORDANCE TO ASTM C 913.
- ② CONTRACTOR TO PROVIDE DRAWING(S) STAMPED BY A PROFESSIONAL ENGINEER FOR STEEL REINFORCING DESIGN FOR CAST-IN-PLACE STRUCTURES.
- ③ 1" CONCRETE KEY POURED AFTER INSTALLATION. 2" SUMP MEASURED FROM TOP OF KEY.

CATCH BASIN COVER MATRIX

CATCH BASIN SIZE	INLET COVER TYPE		F	ALL H'S
	WIDTH (W) (FT)	LENGTH (L) (FT)		
2X3-FT	2	3		X
2.5X3-FT	2.5	3	X	

PIPE MATRIX

CATCH BASIN SIZE	MAXIMUM INSIDE PIPE DIAMETER FOR TWO PIPES	
	WIDTH (IN)	LENGTH (IN)
2X3-FT	12	24
2.5X3-FT	18	24

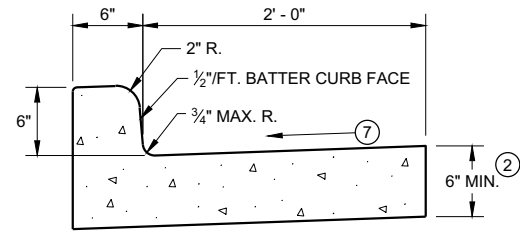


DETAIL "A"

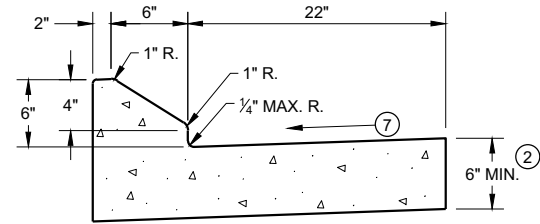
CATCH BASINS 2X3-FT AND 2.5X3-FT

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

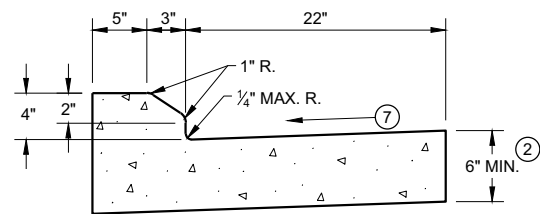
APPROVED
Sep 2016 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
FHWA UNIT SUPERVISOR



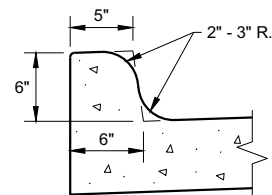
TYPES A^① & D



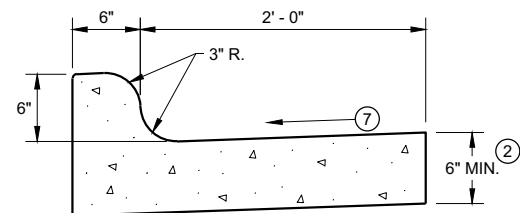
6" SLOPED CURB TYPES G^① & J



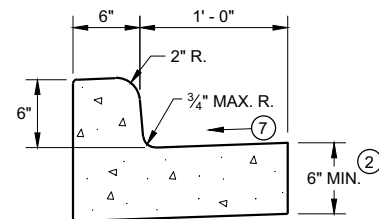
4" SLOPED CURB TYPES G^① & J



TYPES K^① & L
(OPTIONAL CURB SHAPE)

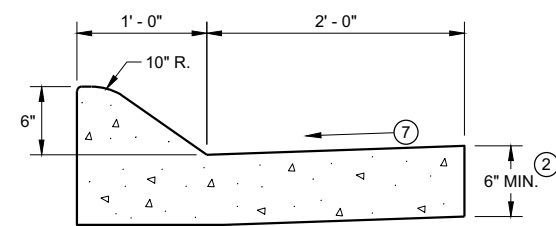


TYPES K^① & L
CONCRETE CURB AND GUTTER 30"

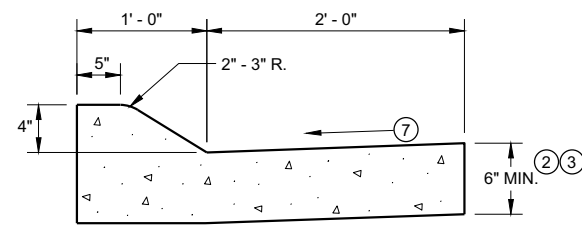


TYPES A^① & D

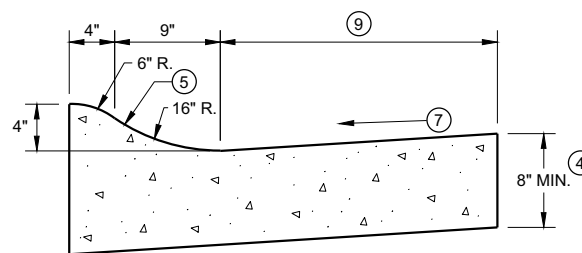
CONCRETE CURB AND GUTTER 18"



6" SLOPED CURB TYPES A^① & D

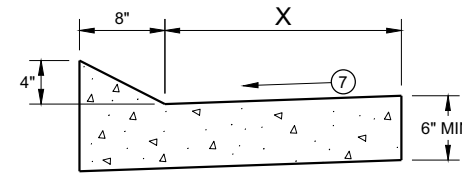


4" SLOPED CURB TYPES A^① & D
CONCRETE CURB AND GUTTER 36"



4" SLOPED CURB TYPES R^① & T

TBT & TBTT	X
30"	22"
36"	28"

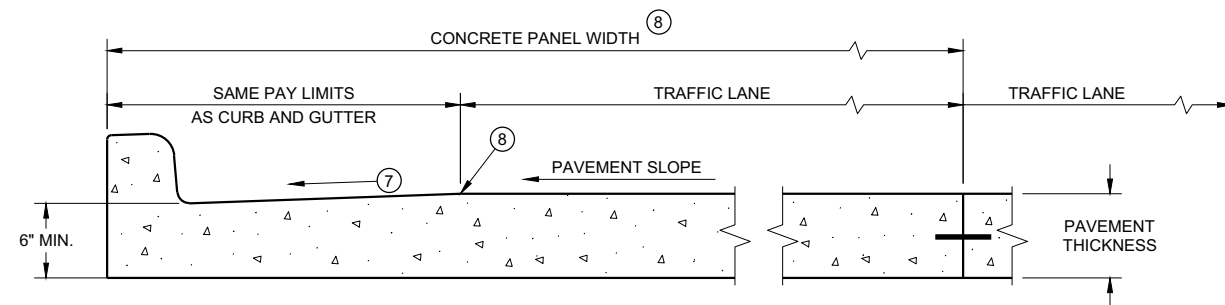


TYPES TBT & TBTT^①

CONCRETE CURB AND GUTTER

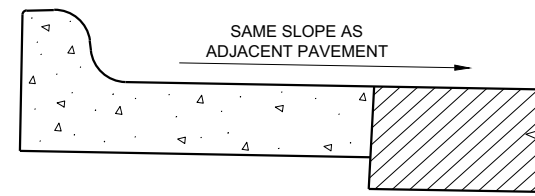
PAVEMENT THICKNESS
AND MAXIMUM CONCRETE
PANEL WIDTH TABLE

PAVEMENT THICKNESS	MAXIMUM PANEL WIDTH
LESS THAN 10"	12'
10" & ABOVE	15'



PARTIAL SECTION OF PAVEMENT *
WITH INTEGRAL CURB AND GUTTER

* BIKE LANE IS NOT SHOWN



REVERSE SLOPE GUTTER^⑥
(TYPICAL FOR ALL CURB & GUTTER TYPES)

GENERAL NOTES

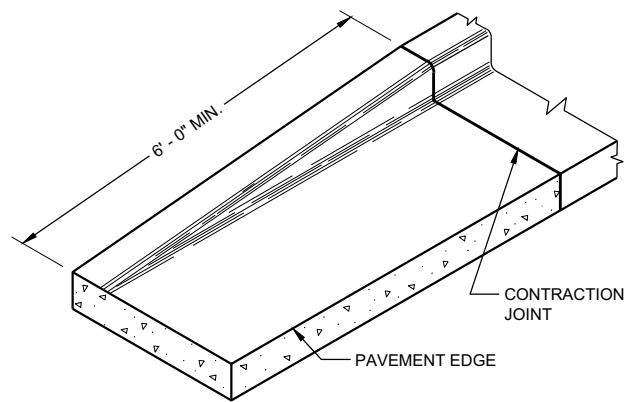
DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

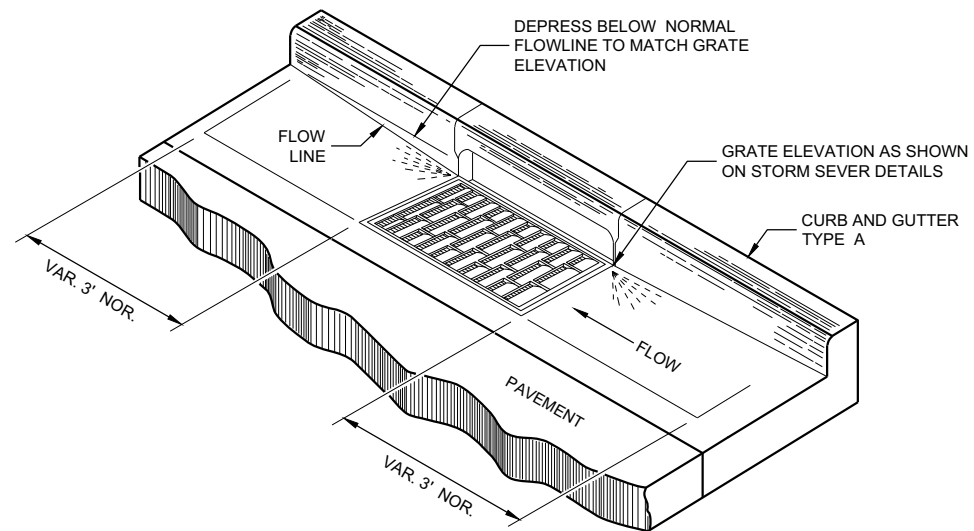
INTEGRAL CURB AND GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB AND GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ③ USE 8" MINIMUM GUTTER THICKNESS WHEN USED WITH AN ADJACENT CONCRETE TRUCK APRON PLACED BEHIND BACK OF CURB.
- ④ THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑤ UNLESS OTHERWISE NOTED, FOR STAKING PURPOSES THE FACE OF CURB IS 6" FROM THE BACK OF CURB.
- ⑥ WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.
- ⑦ USE 4% GUTTER CROSS SLOPE UNLESS OTHERWISE NOTED IN THE PLANS.
- ⑧ INCLUDE LONGITUDINAL JOINT AND TIE BARS ALONG LANE EDGE WHEN CONCRETE PANEL WIDTH EXCEEDS THE MAXIMUM WIDTH PER TABLE BELOW. LONGITUDINAL JOINT(S) ARE NOT ALLOWED WITHIN TRAFFIC LANES AND BIKE LANES. LONGITUDINAL JOINT MAY BE SAWED.
- ⑨ CONCRETE CURB AND GUTTER 4-INCH SLOPED 30-INCH TYPE "R" AND "T" = 17 INCHES
CONCRETE CURB AND GUTTER 4-INCH SLOPED 36-INCH TYPE "R" AND "T" = 23 INCHES



END SECTION CURB AND GUTTER



DETAIL OF CURB AND GUTTER AT INLETS
(TYPICAL H INLET COVER SHOWN)

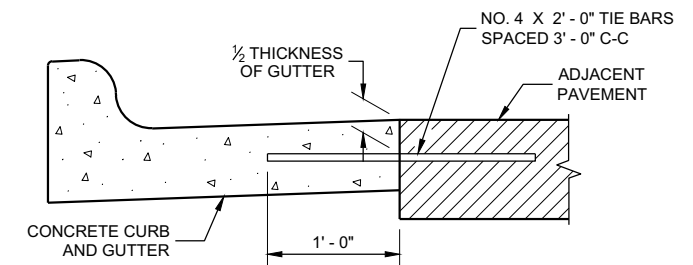
GENERAL NOTES

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

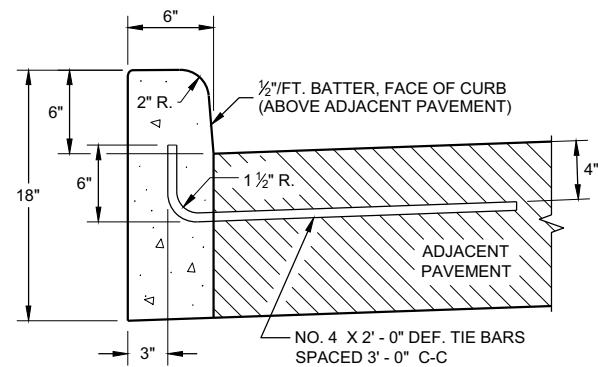
PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2'-0" BEHIND THE BACK OF CURBS.

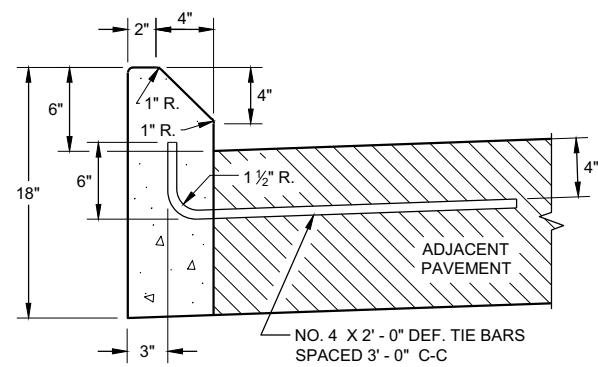
- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑨ REFER TO SDD 08D18 AND 08D19 FOR ADDITIONAL DRIVEWAY ENTRANCE CURB DETAILS.



TYPICAL TIE BAR LOCATION ①

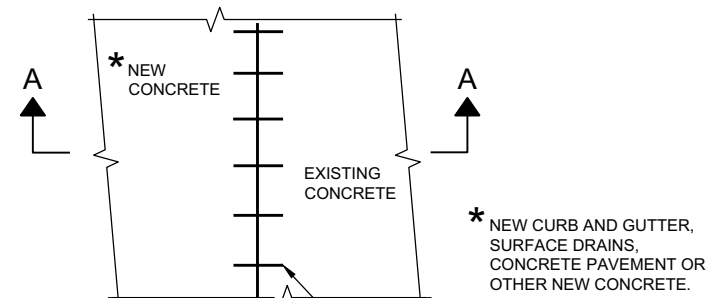


TYPES A ① & D

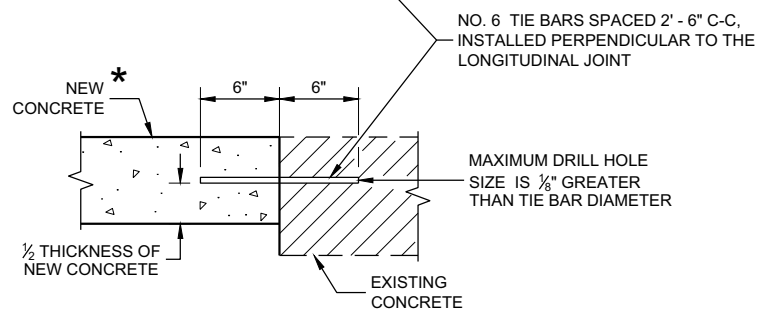


TYPES G ① & J

CONCRETE CURB

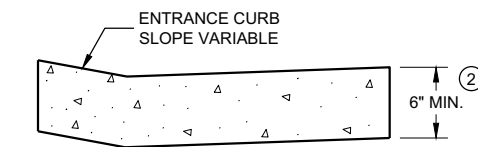


PLAN VIEW



SECTION A - A

TIE BARS DRILLED INTO EXISTING PAVEMENT



DRIVEWAY ENTRANCE CURB ⑨
(WHEN DIRECTED BY THE ENGINEER)

CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS

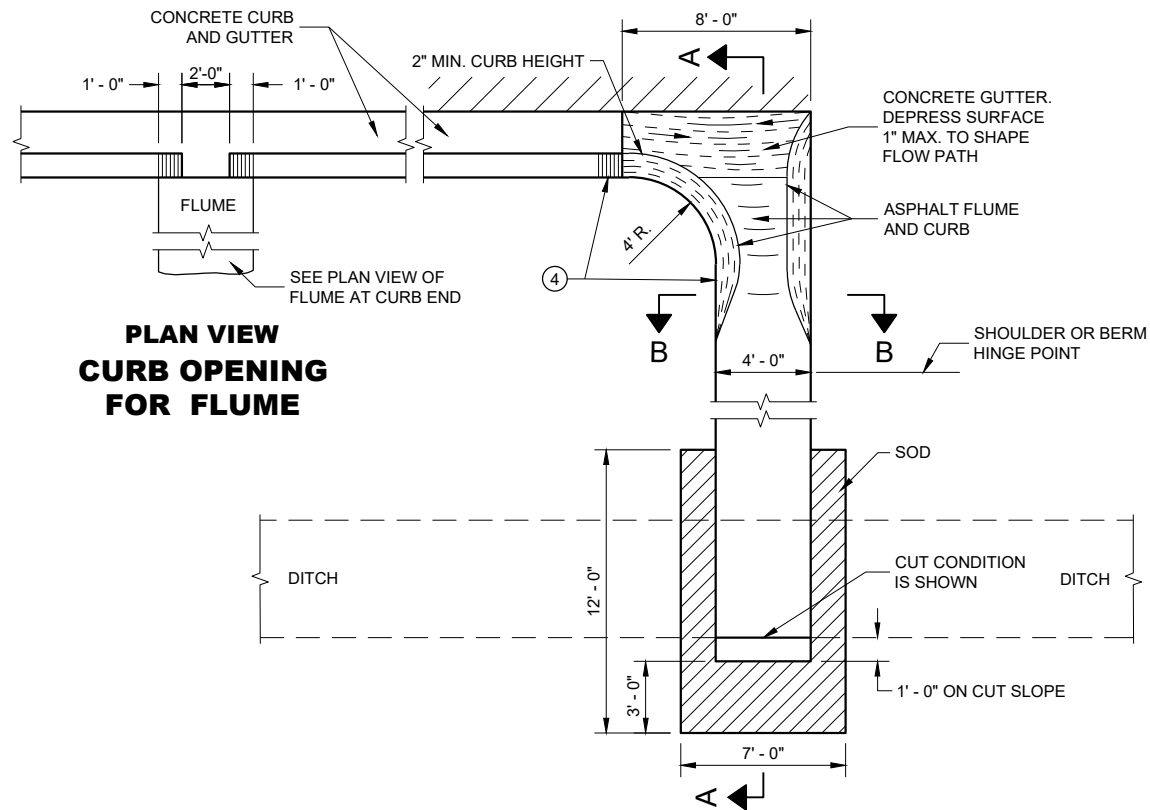
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2021 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT ENGINEER

FHWA

NOTE: TAPER CURB ENDS TO GUTTER IN 1' - 0"

ASPHALTIC FLUME



**PLAN VIEW
CURB OPENING
FOR FLUME**

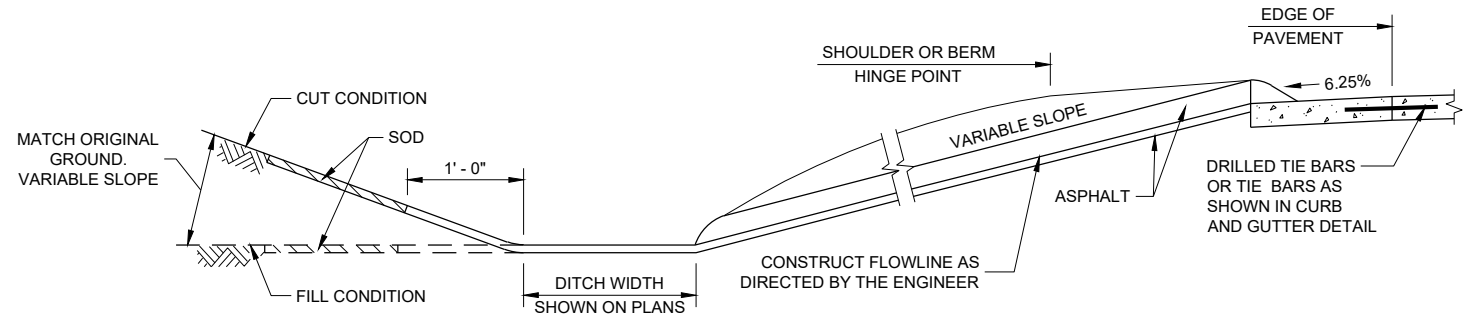
**PLAN VIEW
FLUME AT CURB END**

GENERAL NOTES

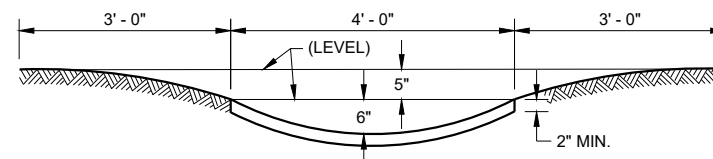
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

4" X 4" - W3.0 X W3.0 CONCRETE REINFORCEMENT SHALL BE IN ACCORDANCE WITH AASHTO SPECIFICATION M55.

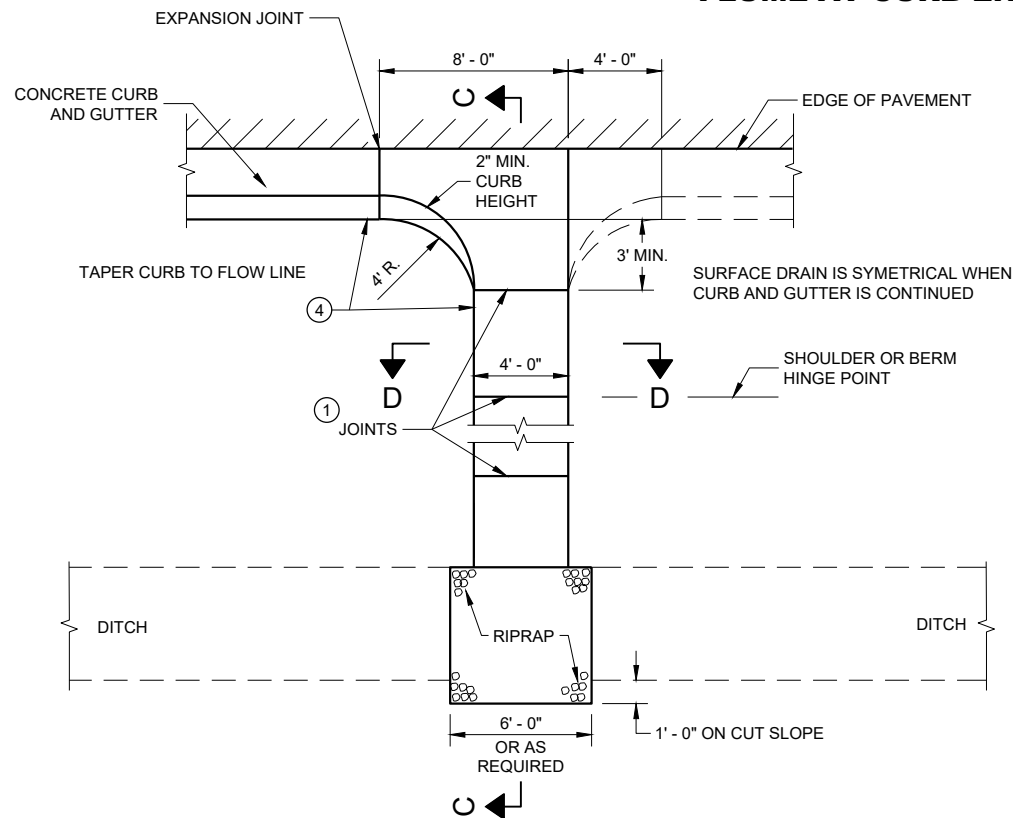
- ① JOINTS SHALL BE 1/8" TO 1/4" WIDE BY 1 1/2" DEEP AND SPACED AT UNIFORM INTERVALS OF APPROXIMATELY 4 FEET.
- ② GEOTEXTILE TYPE "R" SHALL UNDERLAY THE FULL LENGTH AND WIDTH OF THE CONCRETE SURFACE DRAIN AND RIPRAP.
- ③ CONCRETE SURFACE DRAIN WITHOUT CURB AND GUTTER MAY BE USED ON BACKSLOPES WHEN SPECIFIED.
- ④ ANGLE OF FLUME IN RELATION TO BACK OF CURB TO BE CONSTRUCTED PER THE PLAN DETAILS OR AS DIRECTED BY THE ENGINEER. ANGLE OF FLUME MAY BE OTHER THAN 90 DEGREES AS SHOWN.



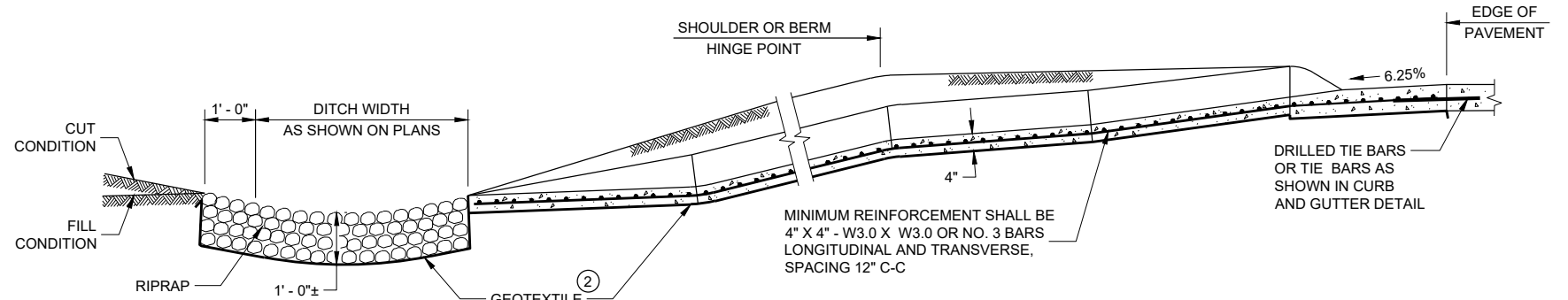
SECTION A - A



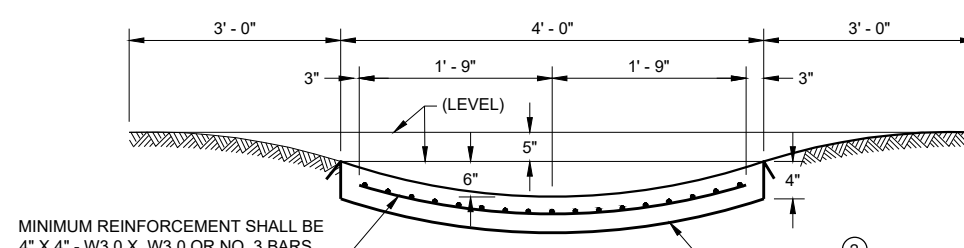
SECTION B - B



**PLAN VIEW
CONCRETE SURFACE DRAIN**



SECTION C - C



SECTION D - D

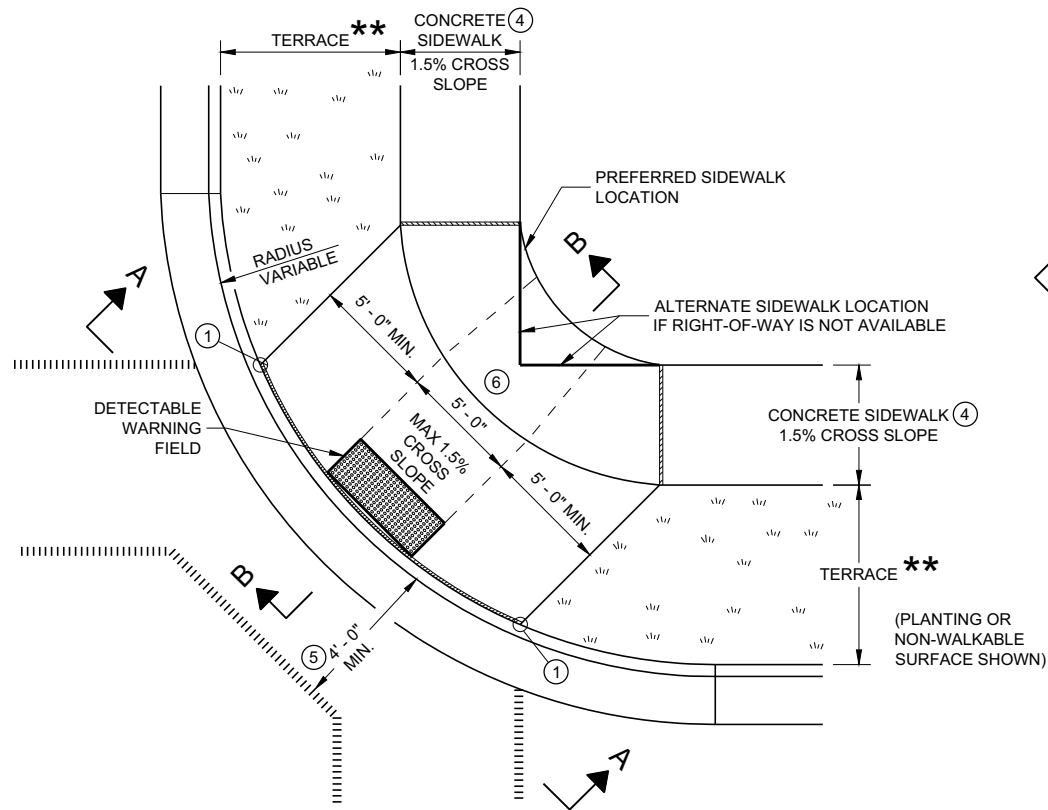
MINIMUM REINFORCEMENT SHALL BE 4" X 4" - W3.0 X W3.0 OR NO. 3 BARS LONGITUDINAL AND TRANSVERSE, SPACING 12" C-C

CONCRETE SURFACE DRAINS AND ASPHALTIC FLUMES

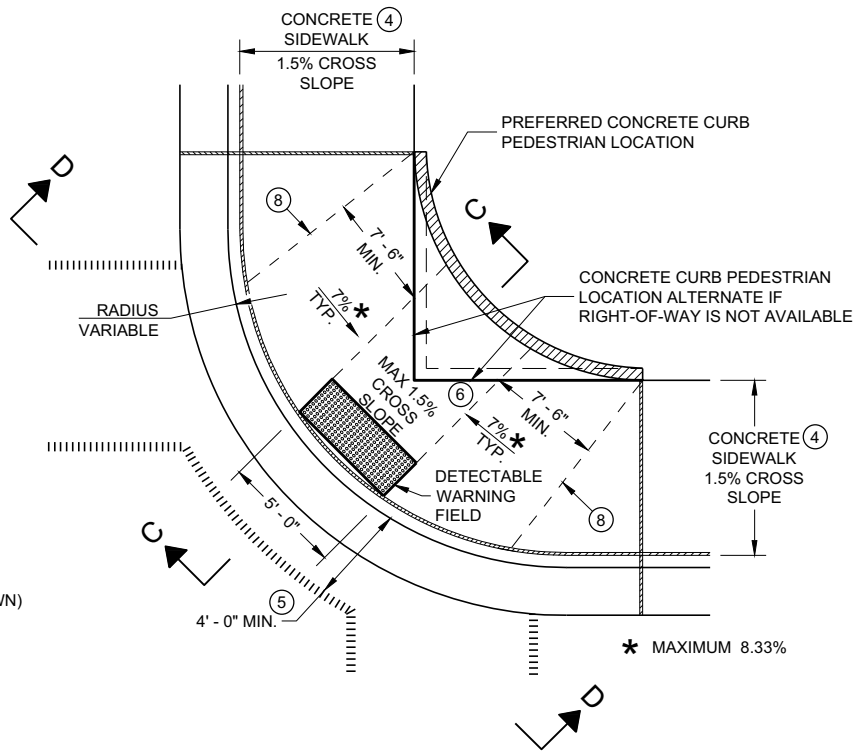
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2021 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
ENGINEER

FHWA



PLAN VIEW
CURB RAMP TYPE 1
(CENTER OF CORNER RADIUS)



PLAN VIEW
CURB RAMP TYPE 1 - A
(NO TERRACE)

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
 DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

WHEN NECESSARY, THE SIDEWALK ELEVATION MAY BE LOWERED TO MEET THE HIGH POINT ON THE RAMP.
 TYPE 1 CURB RAMPS SHALL HAVE A NORMAL SIDEWALK APRON AND CURB ON BOTH SIDES OF RAMP.

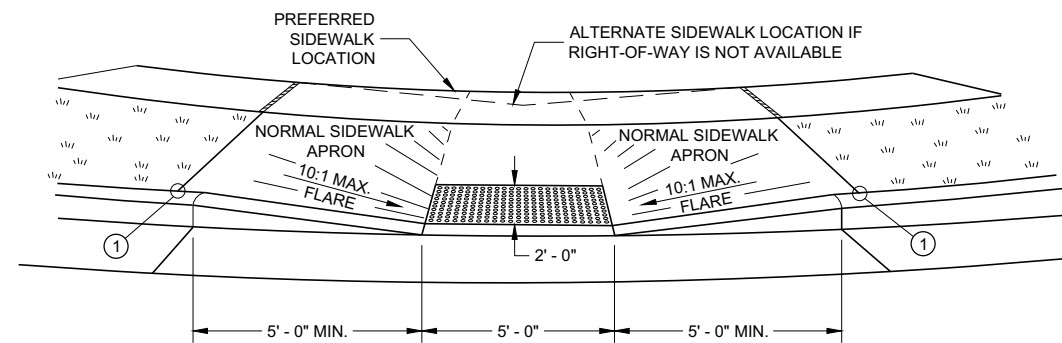
DETECTABLE WARNING FIELD SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS "CURB RAMP DETECTABLE WARNING FIELD". THE CONCRETE PEDESTRIAN CURB, IF NEEDED, SHALL BE MEASURED AND PAID BY THE LINEAR FOOT AS "CONCRETE CURB PEDESTRIAN". CONCRETE SIDEWALK IN THE CURB RAMP AREA SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS CONCRETE SIDEWALK, INCLUDING THE AREA UNDER THE DETECTABLE WARNING FIELD.

SELECT CURB RAMP DETECTABLE WARNING FIELD MATERIALS AND DEVICES FROM THE DEPARTMENT'S APPROVED MATERIALS LIST. THE COLOR OF THE DETECTABLE WARNING FIELD IS SPECIFIED ELSEWHERE AND IS INCIDENTAL TO THE BID ITEM OF "CURB RAMP DETECTABLE WARNING FIELD"

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

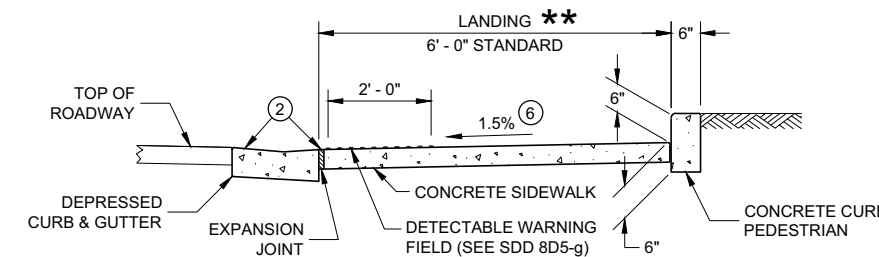
SURFACE TEXTURE OF THE RAMP SHALL BE OBTAINED BY COARSE BROOMING TRANSVERSE TO THE SLOPE OF THE RAMP.

- ① THIS POINT IS AN EXTENSION OF OUTSIDE EDGE OF APPROACHING SIDEWALK WHERE IT MEETS THE BACK OF CONCRETE CURB. POINT LOCATION MAY BE ADJUSTED TO ALIGN WITH BEGINNING OF FULL-HEIGHT CURB IF THIS DISTANCE IS SHORT.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4" INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ MAXIMUM 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑤ PROVIDE A LEVEL LANDING IN THE STREET AND GUTTER AREA (2% MAXIMUM SLOPE IN ANY DIRECTION). WHEN THE GUTTER SLOPE EXCEEDS 2%, CONSTRUCT THE LEVEL LANDING IN THE STREET AREA.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.



VIEW A - A FOR TYPE 1

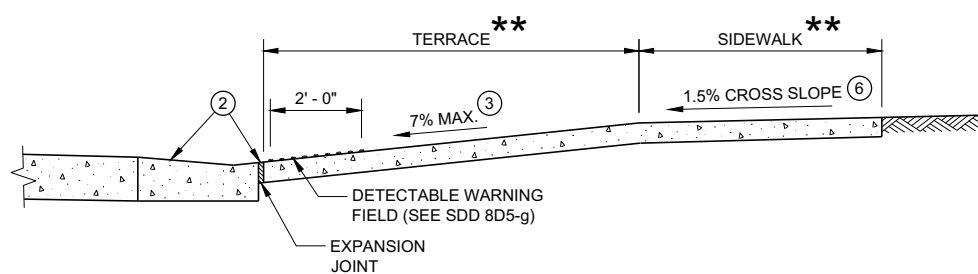
** WIDTH SHOWN ELSEWHERE IN THE PLANS



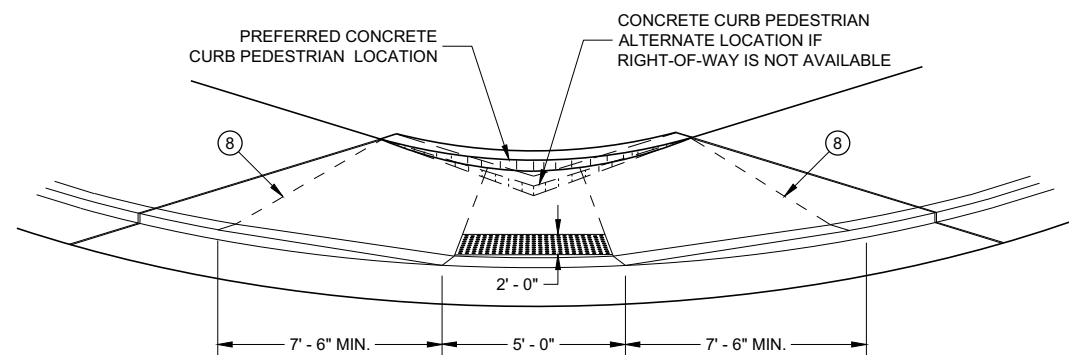
SECTION C - C FOR TYPE 1 - A

LEGEND

- 1/2" EXPANSION JOINT SIDEWALK
- - - - - CONTRACTION JOINT FIELD LOCATED
- ||||||| PAVEMENT MARKING CROSSWALK (WHITE)



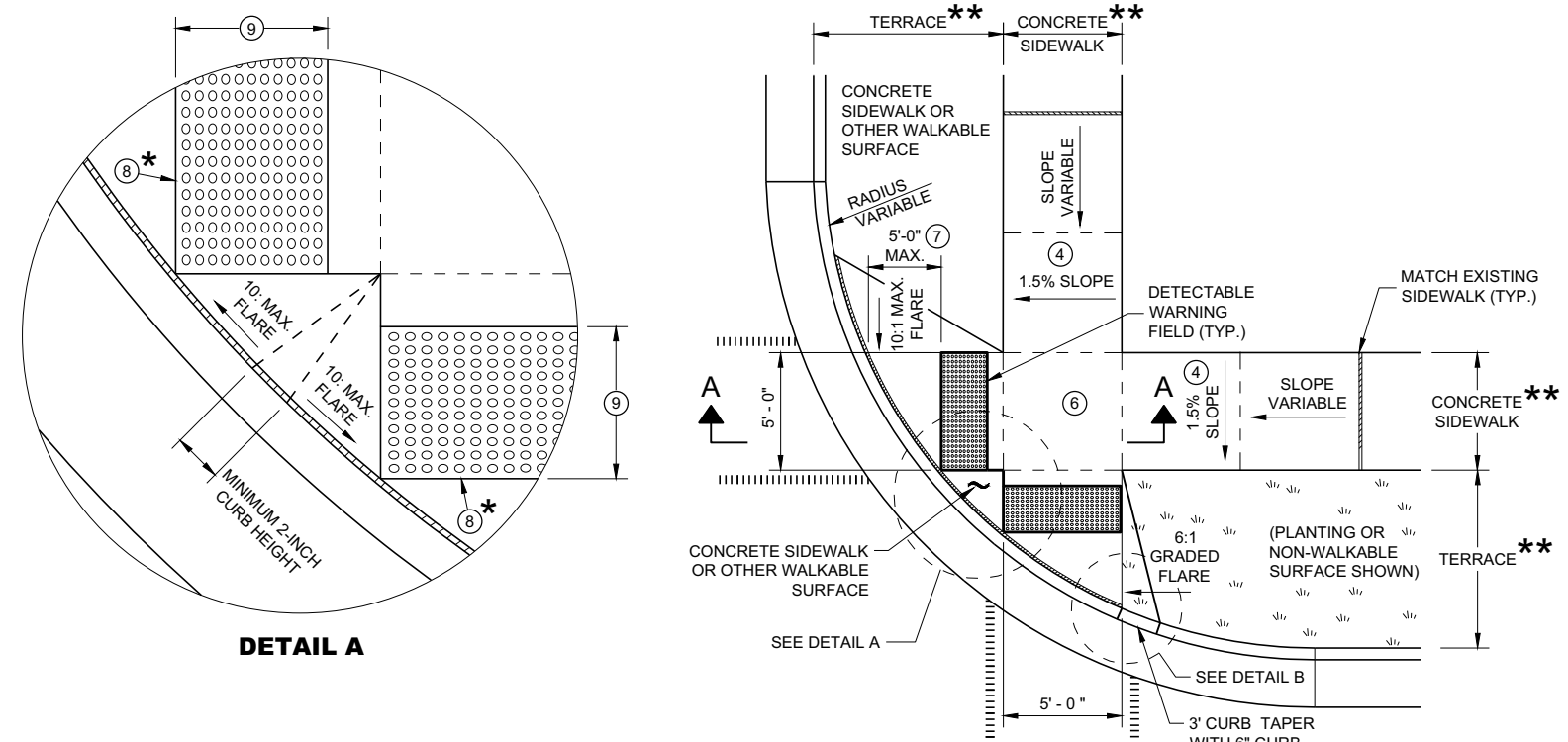
SECTION B - B FOR TYPE 1



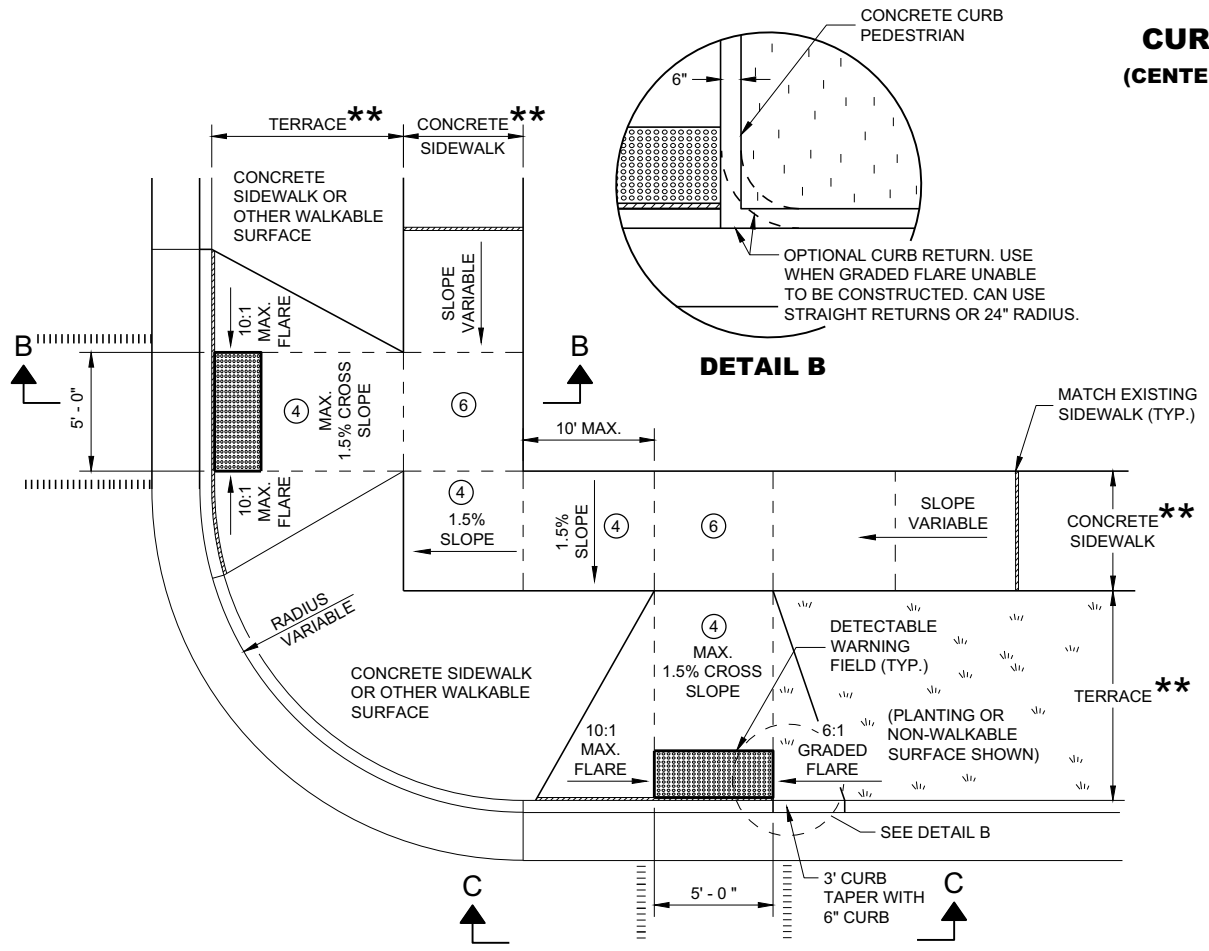
VIEW D - D FOR TYPE 1 - A

CURB RAMPS
TYPE 1 AND 1-A

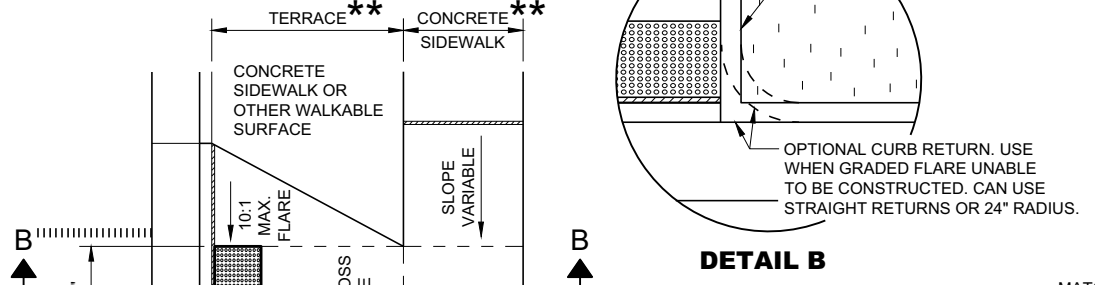
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



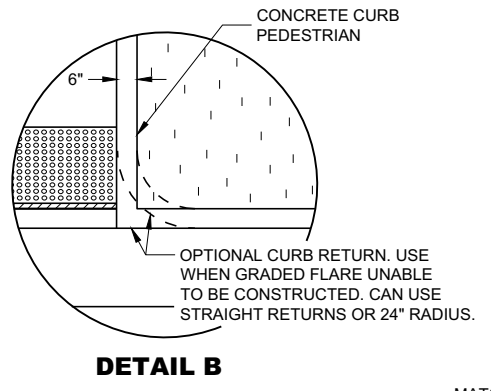
PLAN VIEW CURB RAMP TYPE 2 (CENTER OF CORNER RADIUS)



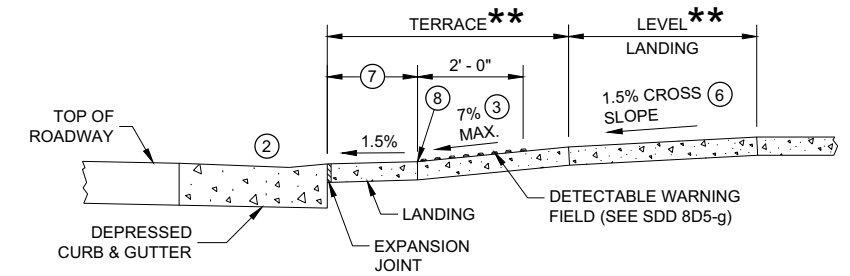
PLAN VIEW CURB RAMP TYPE 3 (OUTSIDE OF CROSSWALK AREA)



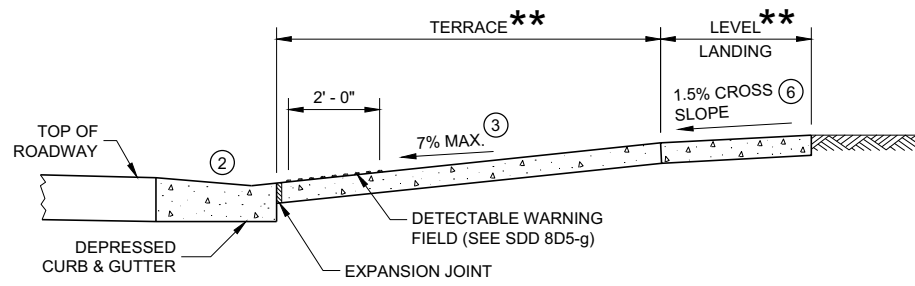
DETAIL A



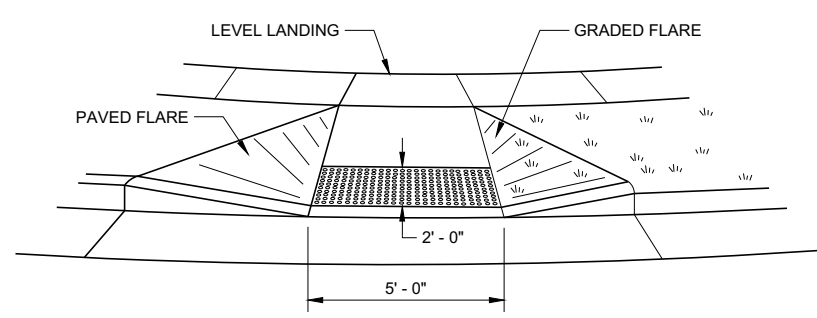
DETAIL B



SECTION A - A FOR TYPE 2



SECTION B - B FOR TYPE 3



VIEW C - C FOR TYPE 3

GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE (2.67% OR LESS) AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET X 5 FEET.
- ⑦ WHEN GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑨ WHEN DISTANCE IS LESS THAN 6' - 0", IT MAY BE DIFFICULT TO ACHIEVE A 7% SLOPE OR FLATTER ALONG THE RAMP. REDUCE CURB HEIGHT IN TRIANGLE AREA TO ACHIEVE 7% SLOPE OR FLATTER ON RAMP. CONSTRUCT 2-INCH MINIMUM CURB HEIGHT BETWEEN 10:1 FLARES.

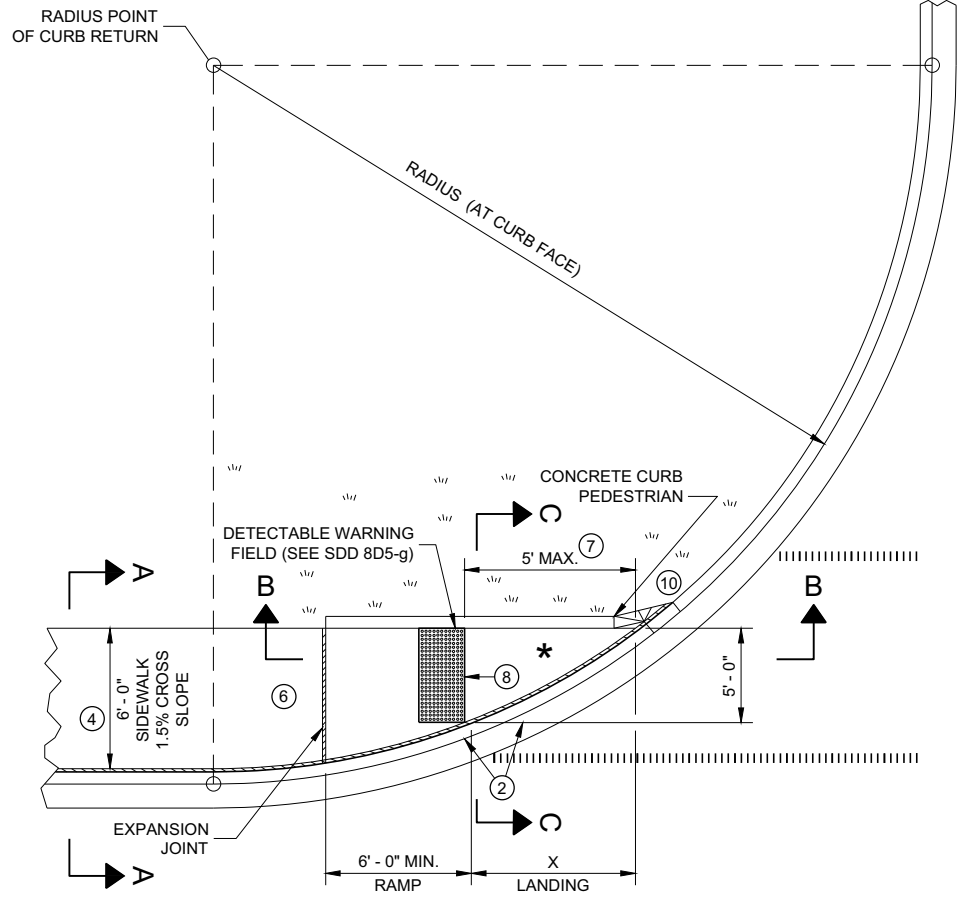
- * MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK
- ** WIDTH SHOWN ELSEWHERE IN THE PLANS

LEGEND

- 1/2" EXPANSION JOINT SIDEWALK
- - - CONTRACTION JOINT SIDEWALK
- ||||| PAVEMENT MARKING CROSSWALK (WHITE)

**CURB RAMPS
TYPE 2 AND 3**

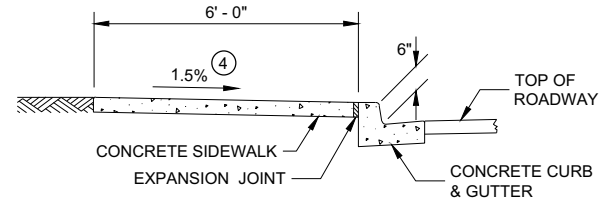
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



PLAN VIEW CURB RAMP TYPE 4A

RADIUS (AT CURB FACE)	X
10 FEET	4' - 7"
15 FEET	6' - 5 1/2"

INTERMEDIATE RADII CAN BE INTERPOLATED



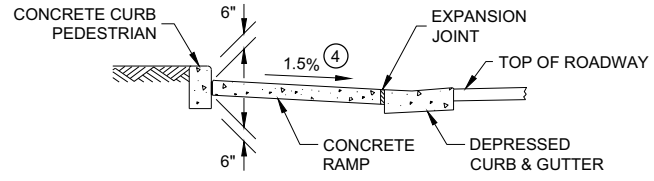
SECTION A - A FOR TYPE 4A

GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4" INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑦ WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑩ INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

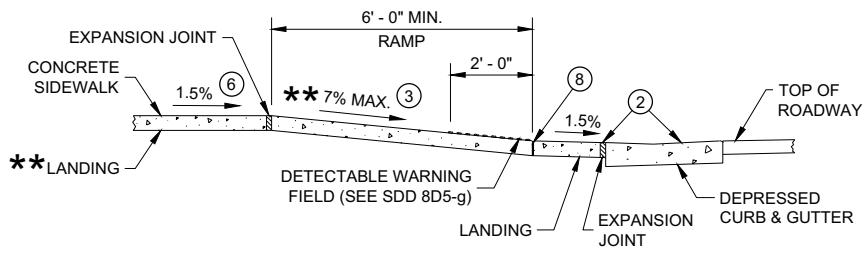
LEGEND

- 1/2" EXPANSION JOINT SIDEWALK
- - - CONTRACTION JOINT SIDEWALK
- ||||| PAVEMENT MARKING CROSSWALK (WHITE)



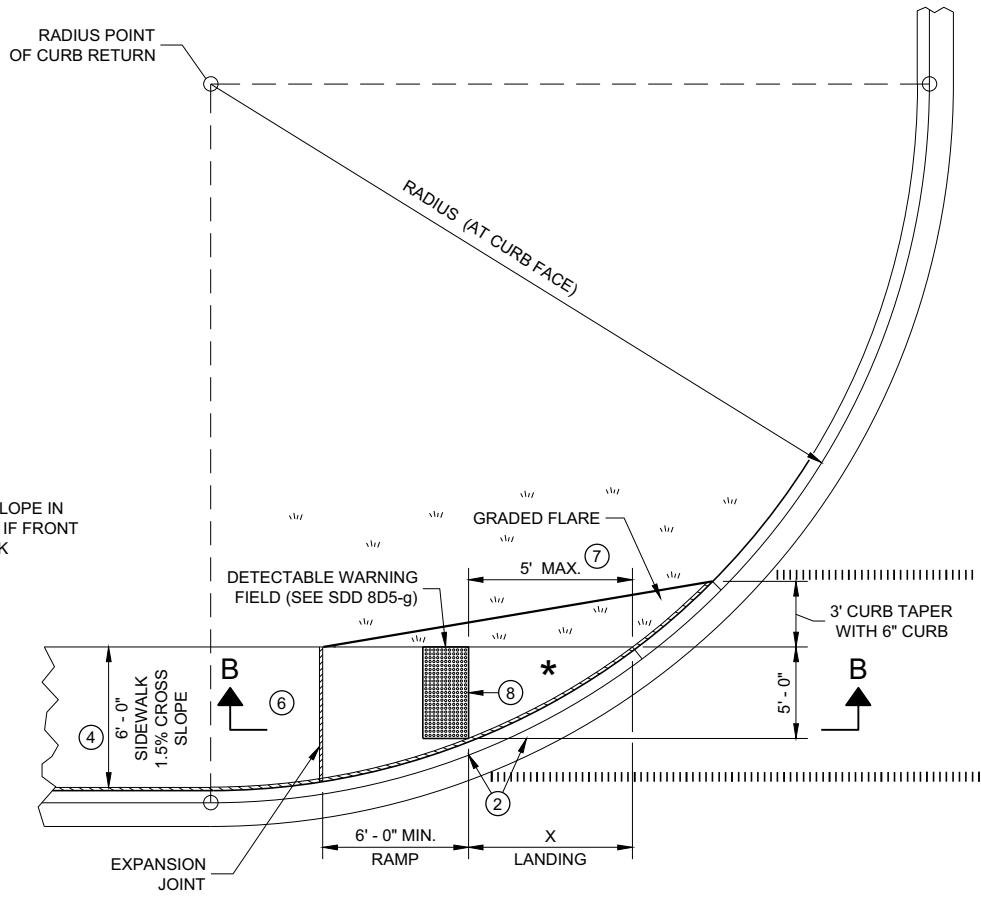
SECTION C - C FOR TYPE 4A

* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IF FRONT OF GRADE BREAK

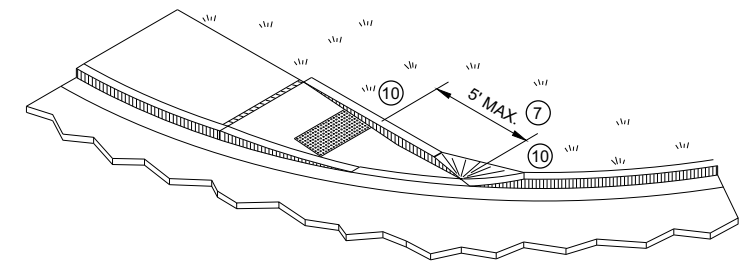


SECTION B - B FOR TYPE 4A AND TYPE 4A1

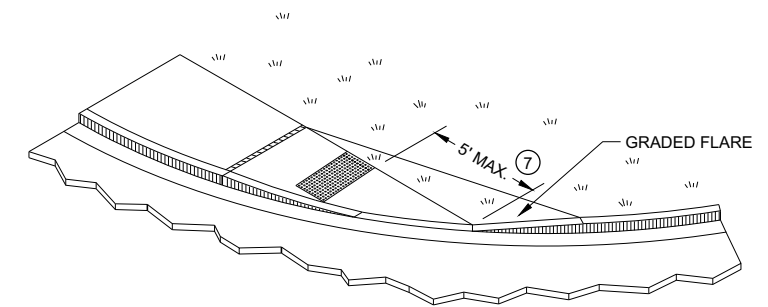
** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED



PLAN VIEW CURB RAMP TYPE 4A1



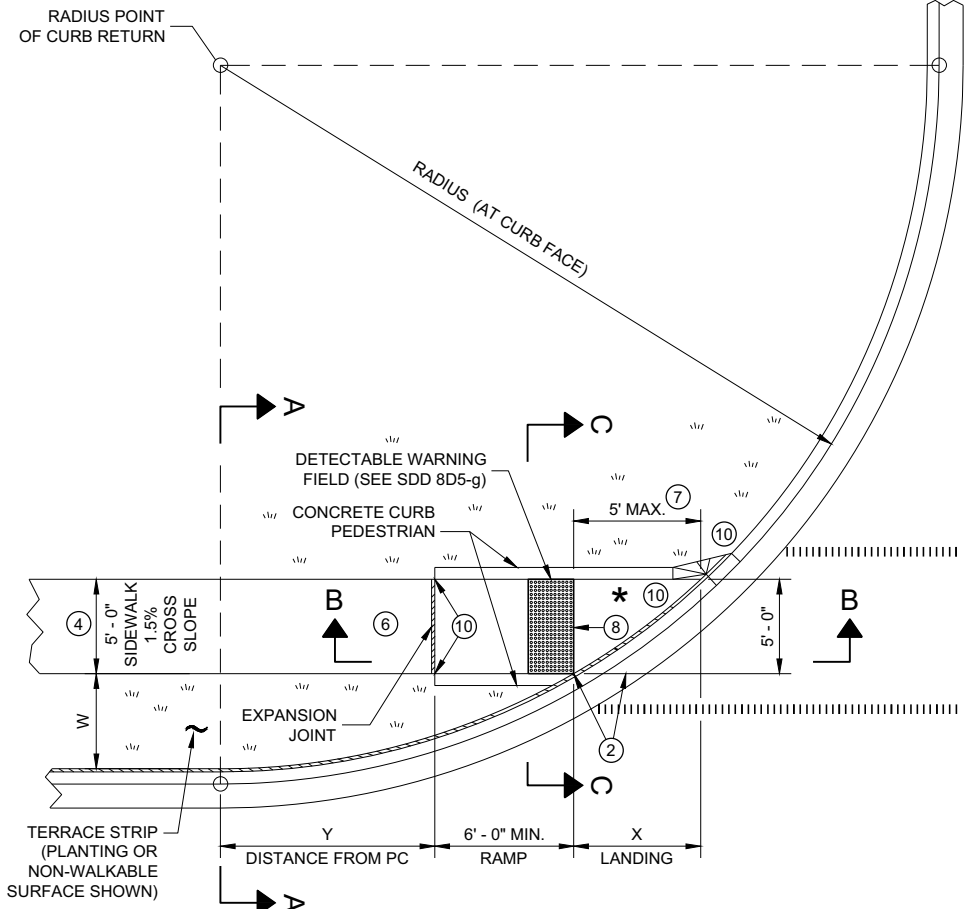
ISOMETRIC VIEW FOR TYPE 4A



ISOMETRIC VIEW FOR TYPE 4A1

CURB RAMPS TYPE 4A AND 4A1

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



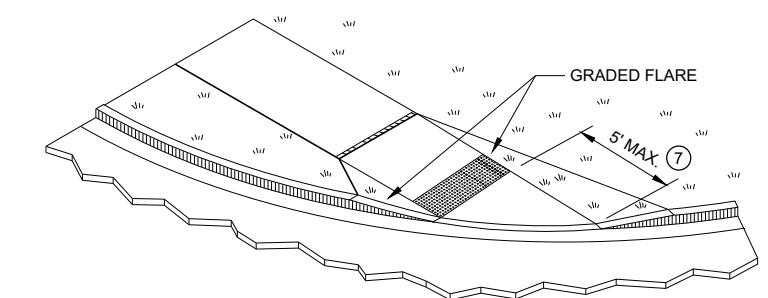
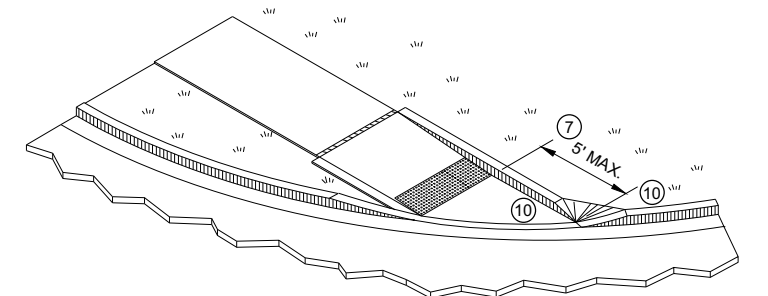
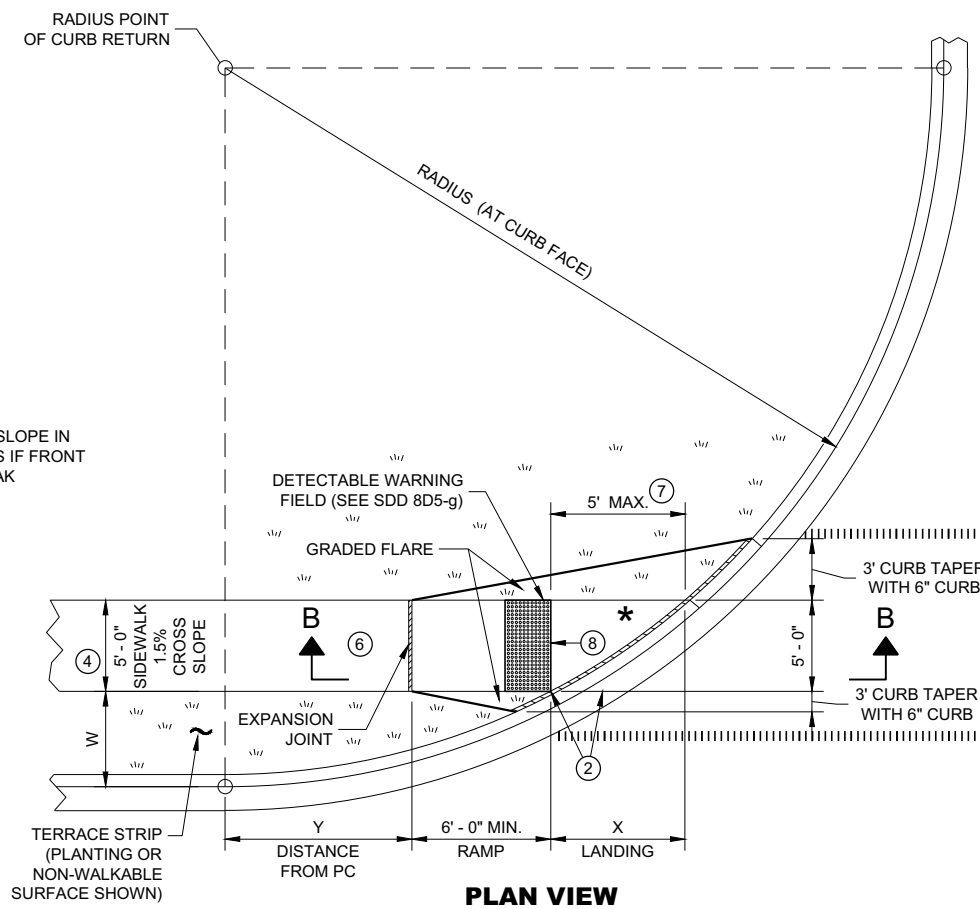
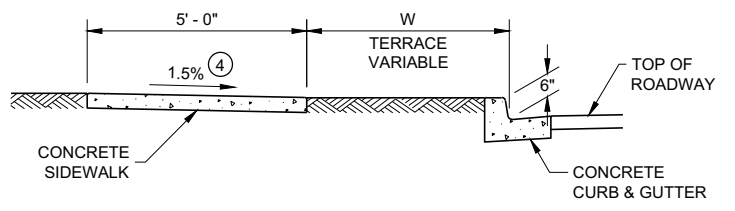
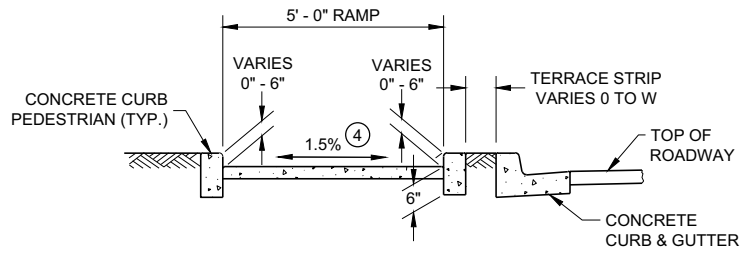
RADIUS (AT CURB FACE)	W = 3' - 0"		W = 4' - 0"		W = 5' - 0"		W = 6' - 0"		W = 7' - 0"		W = 8' - 0"		W = 9' - 0"		W = 10' - 0"	
	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y
10 FEET	2' - 10 1/4"	0' - 5"	2' - 1"	1' - 4 1/2"	1' - 5"	2' - 1"	0' - 10"	2' - 7 1/2"	0' - 3 1/4"	3' - 0 1/4"						
15 FEET	4' - 6 3/4"	2' - 1 3/4"	3' - 9"	3' - 5 3/4"	3' - 1 1/4"	4' - 6"	2' - 6 3/4"	5' - 4 1/2"	2' - 1"	6' - 1"	1' - 8"	6' - 8 1/2"	1' - 3 1/4"	7' - 2 1/2"	0' - 10 3/4"	7' - 7 1/4"
20 FEET	5' - 9 3/4"	3' - 6 1/2"	4' - 11 1/2"	5' - 1 3/4"	4' - 3 1/4"	6' - 5 1/2"	3' - 8 3/4"	7' - 7"	3' - 3"	8' - 6 1/2"	2' - 10"	9' - 4 1/2"	2' - 5 1/2"	10' - 1 1/4"	2' - 1 1/4"	10' - 9"
30 FEET			6' - 9 1/4"	7' - 11 1/4"	6' - 0 1/4"	9' - 8"	5' - 5"	11' - 1 3/4"	4' - 10 3/4"	12' - 5 3/4"	4' - 5 1/2"	13' - 7 3/4"	4' - 0 3/4"	14' - 8 1/2"	3' - 8 1/2"	15' - 8 1/4"
40 FEET									6' - 1 3/4"	15' - 8 1/2"	5' - 8"	17' - 2"	5' - 3"	18' - 5 3/4"	4' - 10 3/4"	19' - 8 1/4"
50 FEET															5' - 10 1/4"	23' - 2"

INTERMEDIATE RADII CAN BE INTERPOLATED
 DIMENSION "Y" IS CALCULATED BASED ON 6'-0" RAMP LENGTH
 DIMENSION "X" IS CALCULATED BASED ON 5'-0" SIDEWALK WIDTH

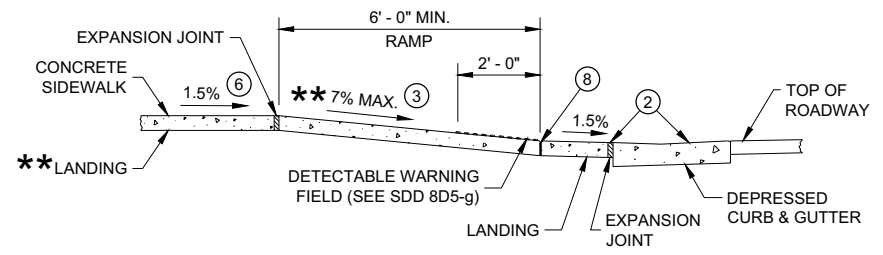
- LEGEND**
- ===== 1/2" EXPANSION JOINT SIDEWALK
 - - - - - CONTRACTION JOINT SIDEWALK
 - ||||||| PAVEMENT MARKING CROSSWALK (WHITE)

GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/2 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑦ WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑩ INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.



* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IF FRONT OF GRADE BREAK



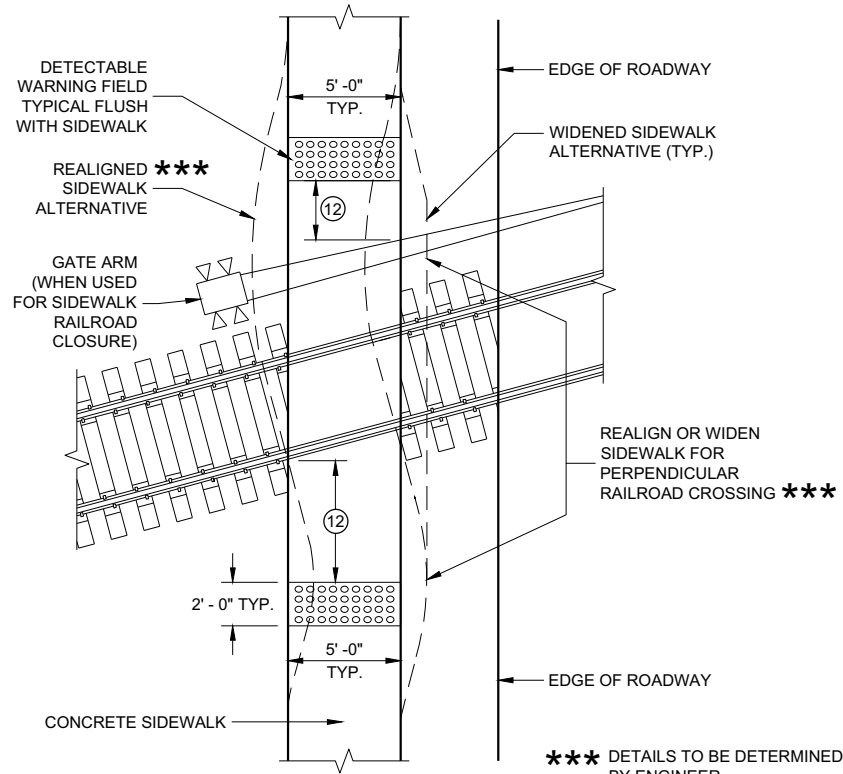
** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

**CURB RAMPS
TYPE 4B AND 4B1**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

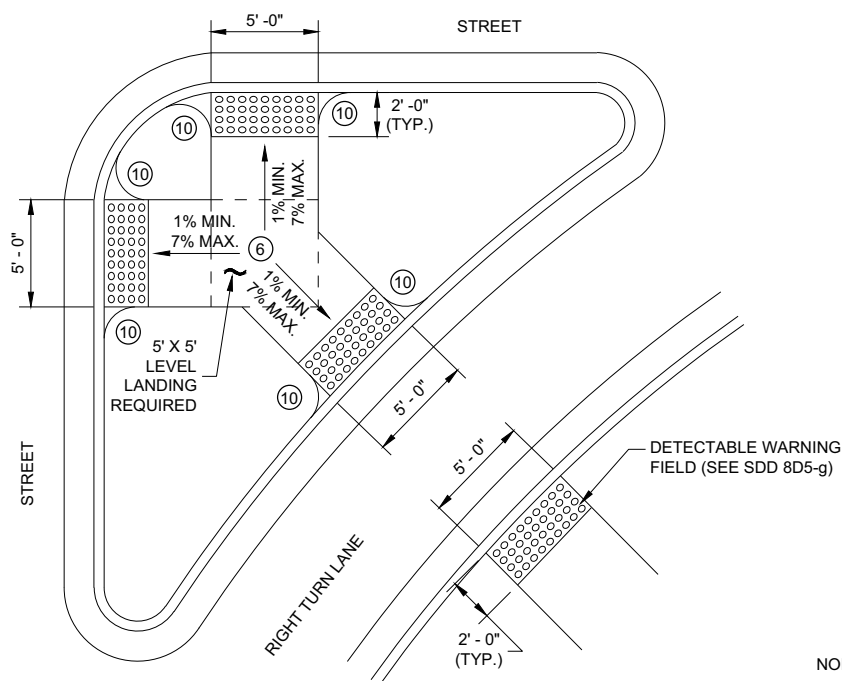
SDD08D05 - 20d

SDD08D05 - 20d



CURB RAMP TYPE 8

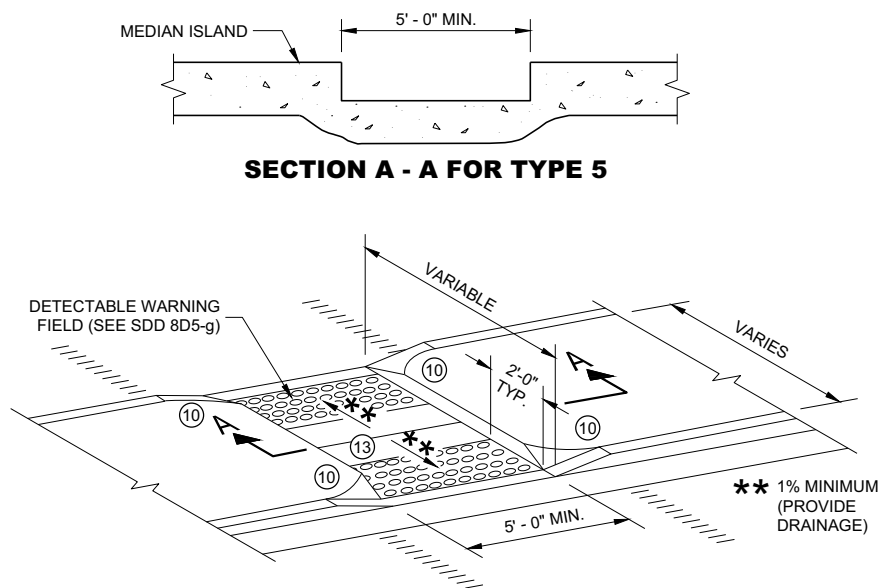
DETECTABLE WARNINGS AT RAILROAD CROSSING



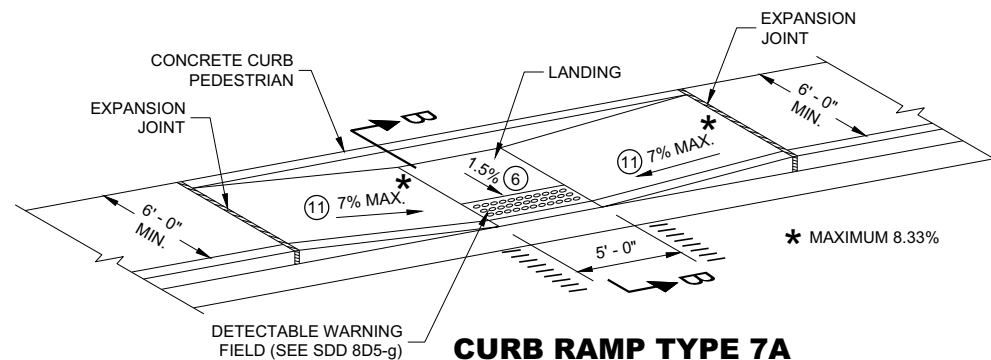
CURB RAMP TYPE 6

DETECTABLE WARNING AT ISLANDS

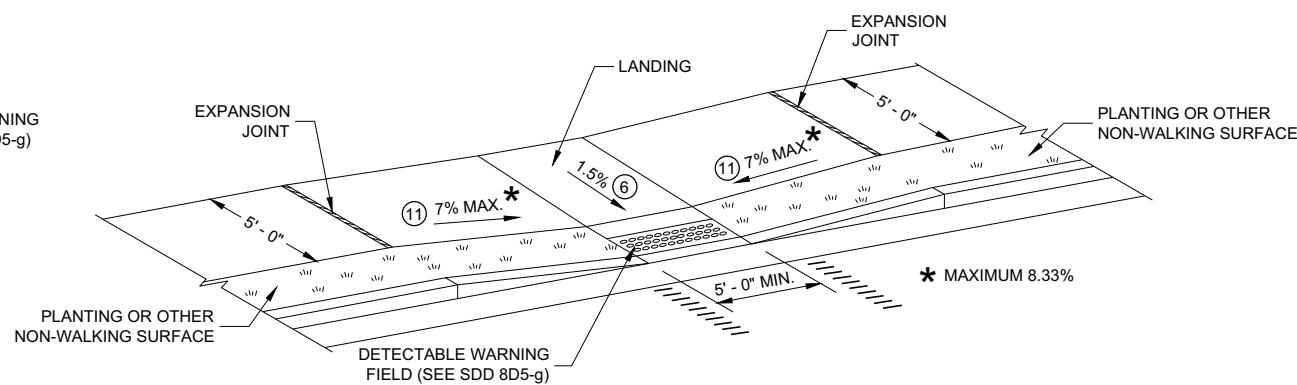
REFER TO GENERAL NOTES (2) AND (3) FOR ALL ISLAND CURB RAMPS



CURB RAMP TYPE 5
MEDIAN ISLAND
NON-ELEVATED PEDESTRIAN CROSSING



CURB RAMP TYPE 7A
MID BLOCK CROSSING



CURB RAMP TYPE 7B
MID BLOCK CROSSING

NOTE: THESE PARALLEL AND PARALLEL/PERPENDICULAR CURB RAMPS MAY BE USED AT INTERSECTIONS AND MID BLOCK LOCATIONS.

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

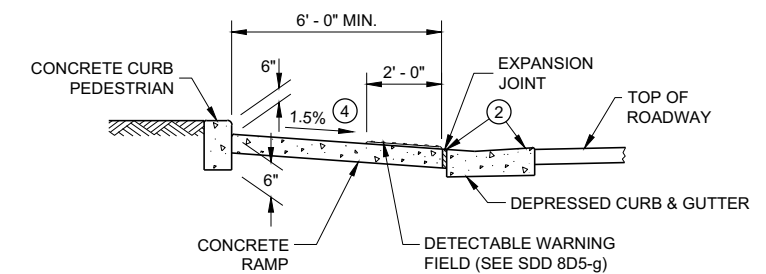
SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

- (2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- (3) AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- (4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- (6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- (10) INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.
- (11) SLOPE SIDEWALK TOWARD LANDING AS SHOWN WHERE THERE IS NO TERRACE OR WHERE THE TERRACE WIDTH IS LESS THAN 6 FEET WIDE.
- (12) THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO A RAILROAD CROSSING SHALL BE 1.5 FEET ±0.1' FROM THE FACE OF THE GATE ARM IF THE GATE ARM EXTENDS ACROSS THE SIDEWALK. WHERE THERE IS NO PEDESTRIAN GATE, THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO THE RAILROAD CROSSING SHALL BE 15 FEET FROM THE NEAREST RAIL.
- (13) DO NOT INSTALL DETECTABLE WARNING FIELDS AT THE EDGES OF STREET-LEVEL PEDESTRIAN REFUGE ISLANDS IF A MINIMUM 2 FOOT CONCRETE SURFACE WITHOUT DETECTABLE WARNINGS (MEASURED IN THE DIRECTION OF PEDESTRIAN TRAVEL) CANNOT BE ACHIEVED.

LEGEND

- ===== 1/2" EXPANSION JOINT SIDEWALK
- - - - - CONTRACTION JOINT FIELD LOCATED
- ||||||| PAVEMENT MARKING CROSSWALK (WHITE)

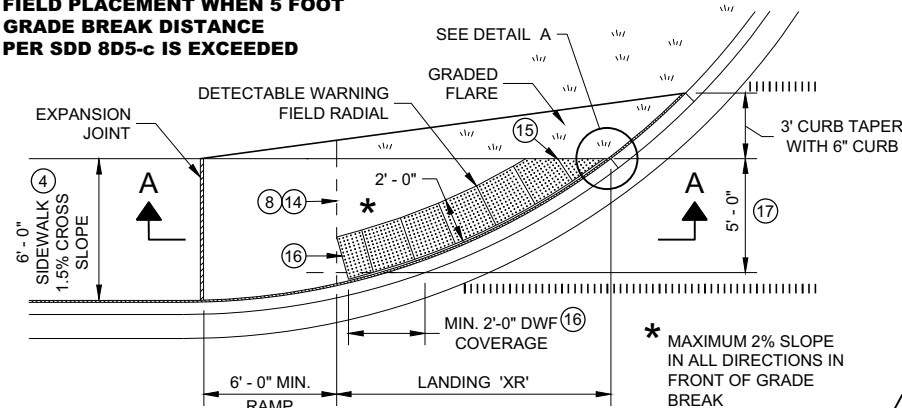


SECTION B - B FOR TYPE 7A

CURB RAMPS
TYPE 5, 6, 7A, 7B & 8

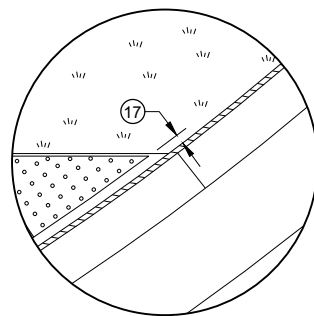
STATE OF WISCONSIN
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RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-c IS EXCEEDED

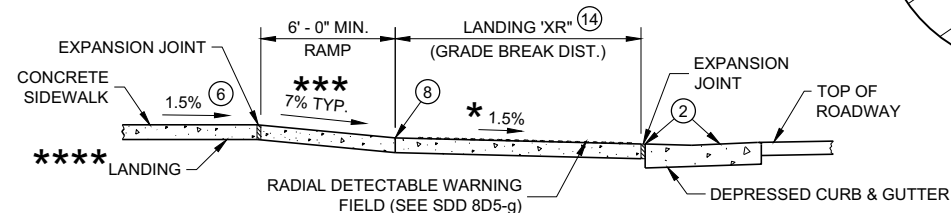


**PLAN VIEW
CURB RAMP TYPE 4A1
(GRADE BREAK DISTANCE GREATER THAN 5 FEET)**

- LEGEND**
- 1/2" EXPANSION JOINT SIDEWALK
 - - - - - CONTRACTION JOINT SIDEWALK
 - ||||| PAVEMENT MARKING CROSSWALK (WHITE)



DETAIL A



SECTION A - A FOR TYPE 4A1

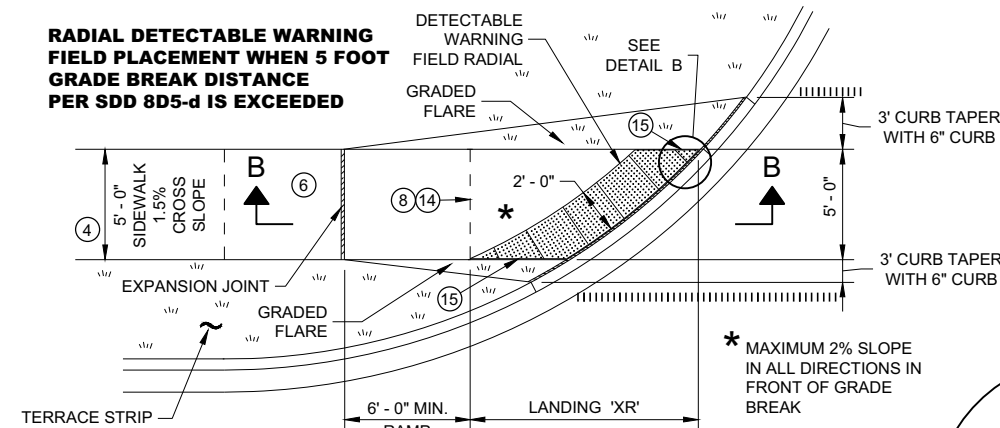
**** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

*** MAXIMUM 8.33%

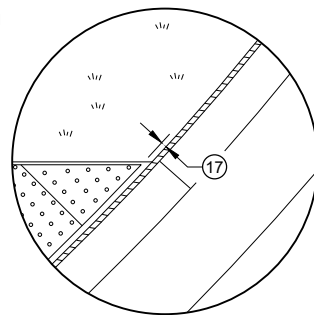
GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- APPLY RADIAL DETECTABLE WARNING PLACEMENT SIMILARLY FOR TYPE 4A AND 4A1 CURB RAMPS AND SIMILARLY FOR TYPE 4B AND 4B1 CURB RAMPS. TYPE 4A AND 4B RAMPS ARE NOT SHOWN.
- REFER TO SDD 8D5-g FOR ADDITIONAL RADIAL PLATE REQUIREMENTS.
- FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FILED ARE PROHIBITED.
- DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑭ CONSULT ENGINEER IF GRADE BREAK LOCATION (END OF LANDING DIMENSION "XR") REQUIRES FIELD ADJUSTMENT WHEN ESTABLISHING FINAL RADIAL DETECTABLE WARNING FIELD LOCATION.
- ⑮ FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN 1/8" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.
- ⑯ USE 1' X 2" RECTANGULAR END PLATE AT END OF TYPE 4A1 RAMP AND PROVIDE MINIMUM 2' - 0" DETECTABLE WARNING FIELD COVERAGE (IN DIRECTION OF PEDESTRIAN TRAVEL) ALONG THE ENTIRE CURB RAMP WIDTH.
- ⑰ A MAXIMUM 3 INCH CONCRETE BORDER WITH IS ALLOWABLE IN FROM OF RADIAL DETECTABLE WARNING FIELD FOR CONSTRUCTABILITY PURPOSES. CONCRETE BORDER WIDTH MAY VARY UP TO 1 INCH.

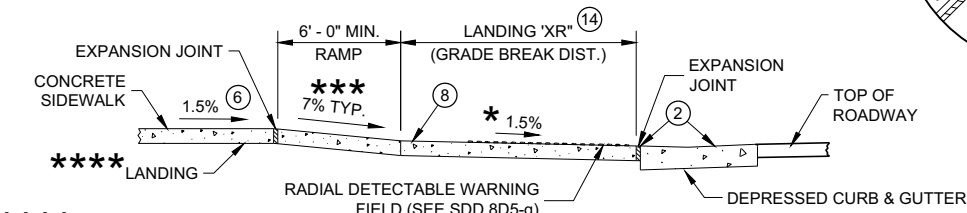
RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-d IS EXCEEDED



**PLAN VIEW
CURB RAMP TYPE 4B1
(GRADE BREAK DISTANCE GREATER THAN 5 FEET)**



DETAIL B

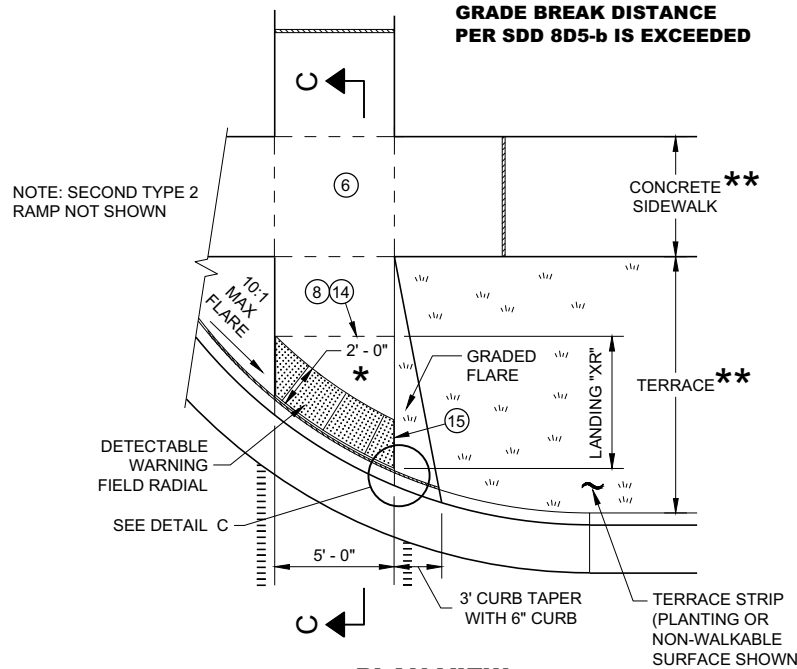


SECTION B - B FOR TYPE 4B1

**** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

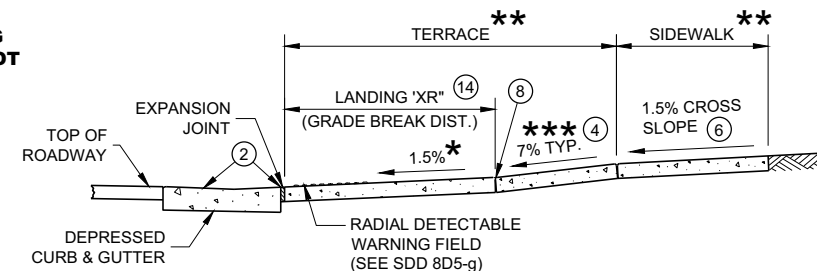
*** MAXIMUM 8.33%

RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-b IS EXCEEDED



**PLAN VIEW
CURB RAMP TYPE 2
(GRADE BREAK DISTANCE GREATER THAN 5 FEET)
(ON LINE WITH SIDEWALK)**

NOTE: SECOND TYPE 2 RAMP NOT SHOWN

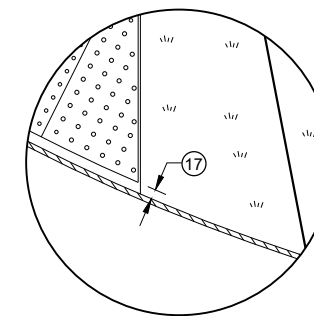


SECTION C - C FOR TYPE 2

* MAXIMUM 2% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK

** WIDTH SHOWN ELSEWHERE IN THE PLANS

*** MAXIMUM 8.33%



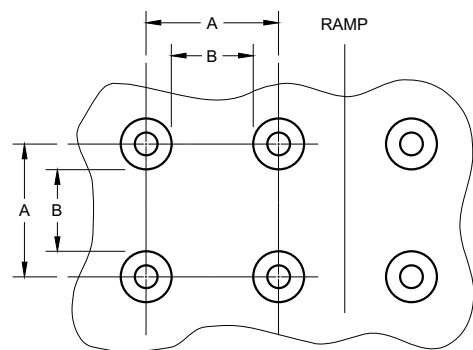
DETAIL C

**CURB RAMPS
RADIAL DETECTABLE WARNING
FIELD APPLICATIONS**

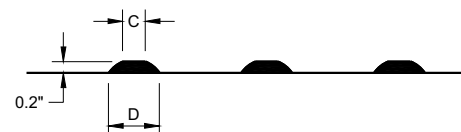
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

	MIN.	MAX.
A	1.6"	2.4"
B	0.65"	1.5"
C	*	*
D	0.9"	1.4"

* THE C DIMENSION IS 50% TO 65% OF THE D DIMENSION.

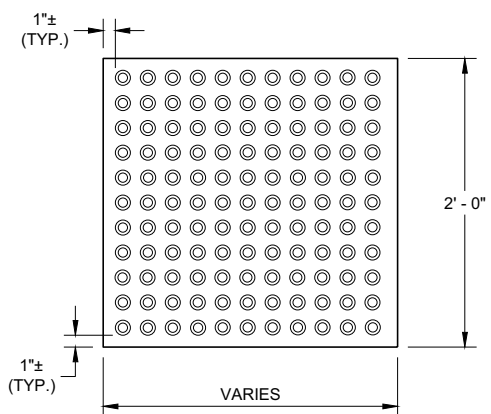


PLAN VIEW

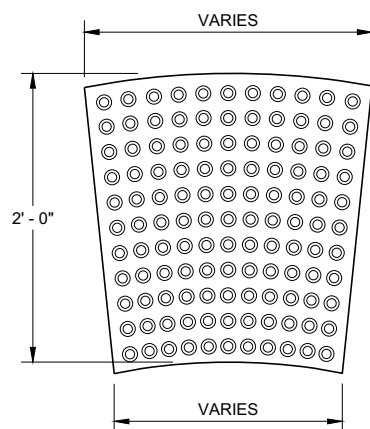


ELEVATION VIEW

**TRUNCATED DOMES
DETECTABLE WARNING PATTERN DETAIL**

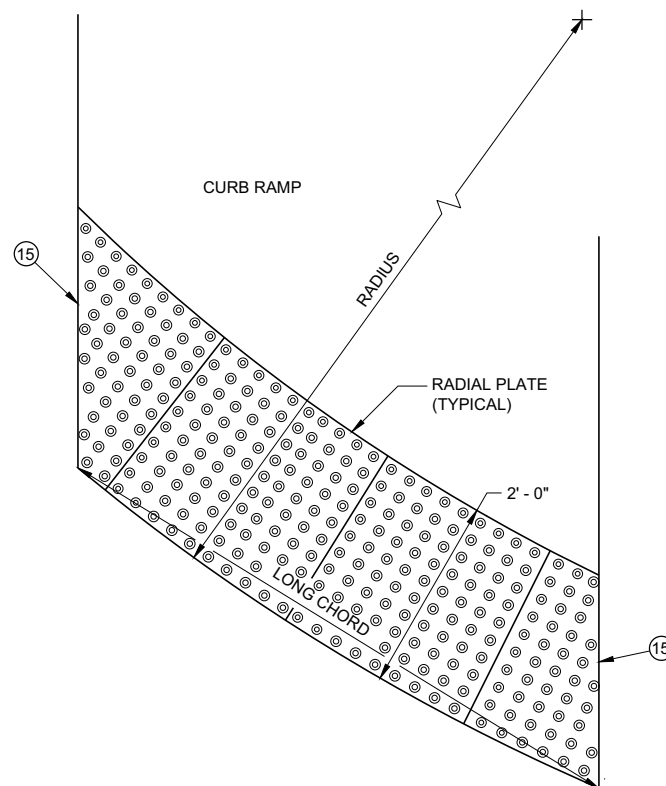


RECTANGULAR
PLATES

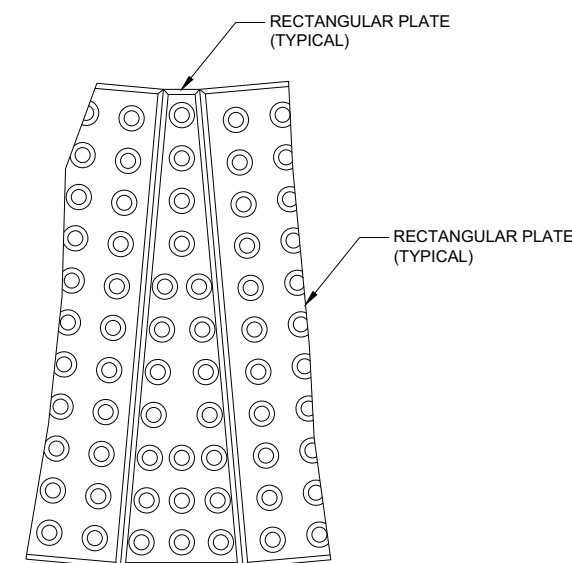


RADIAL
PLATES

PLAN VIEW
DETECTABLE WARNING FIELDS (TYPICAL)



PLAN VIEW
RADIAL DETECTABLE
WARNING FIELD ATTRIBUTES



PLAN VIEW
RADIAL WEDGE PLATE
CONNECTION DETAIL

GENERAL NOTES

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AT A CURB RAMP SHALL BE FROM THE SAME MANUFACTURER.

PLACE ALL DETECTABLE WARNING FIELD SYSTEMS IN ACCORDANCE TO THE MANUFACTURER'S RECOMMENDATION.

FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FIELD ARE PROHIBITED.

DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.

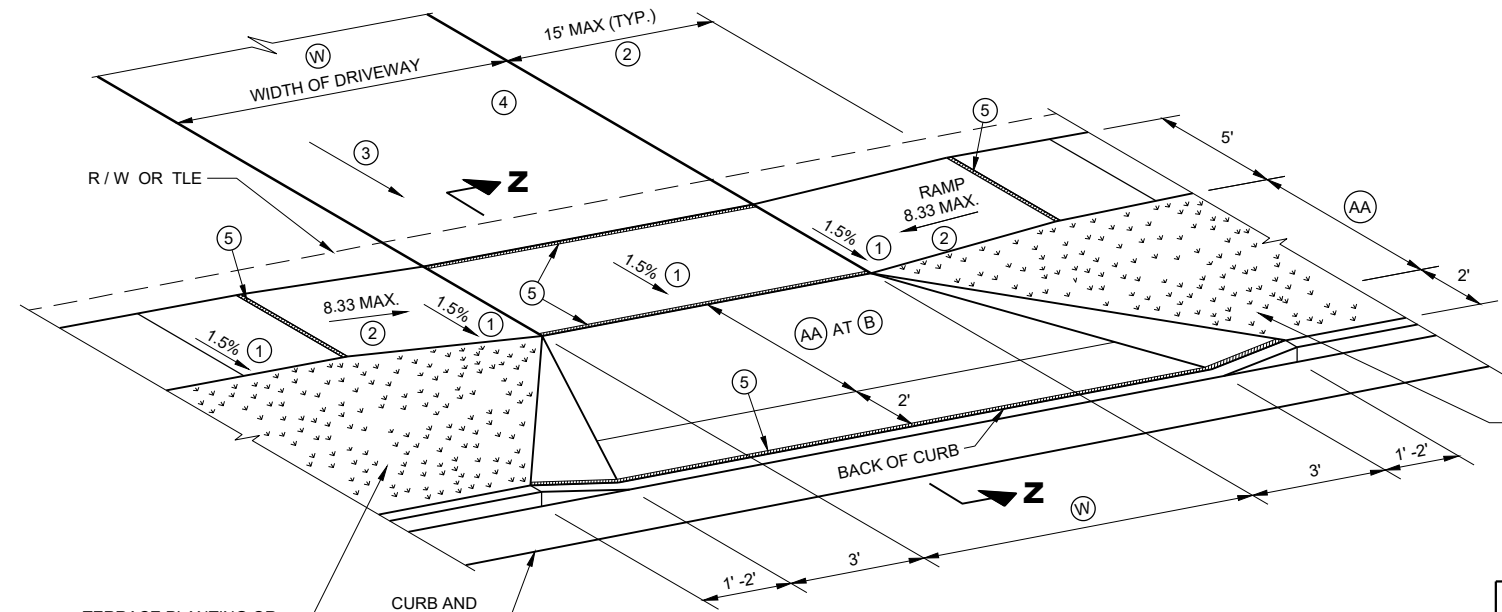
FOR RADIAL DETECTABLE WARNING FIELD APPLICATIONS WHERE STANDARD RADIAL PLATES ARE NOT AVAILABLE AT AN INTERSECTION CURB RADIUS, A COMBINATION OF SQUARE OR RECTANGULAR PLATES AND RADIAL PLATES MAY BE USED TO FORM RADIAL CONFIGURATION. RADIAL WEDGE PLATES IN COMBINATION WITH SQUARE PLATES ARE ALSO ACCEPTABLE. FOLLOW MANUFACTURER'S RECOMMENDATIONS.

REFER TO CONTRACT AND STANDARD SPECIFICATIONS FOR FIELD CUTTING REQUIREMENTS.

DO NOT EMBED IN CONCRETE ANY FIELD-CUT PLATES WITH CUT EDGES SHORTER THAN 6 INCHES. CONSULT WITH MANUFACTURER FOR RE-DRILLING AND ANCHORING REQUIREMENTS OF FIELD-CUT PLATES.

15 FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN 1/8" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.

CURB RAMPS RECTANGULAR AND RADIAL DETECTABLE WARNING PLATES	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2019 DATE	/S/ Rodney Taylor ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR
<small>FHWA</small>	



**TYPE Z
SIDEWALK WITH WIDER TERRACE
TERRACE VARIES 7 TO 12 FEET**

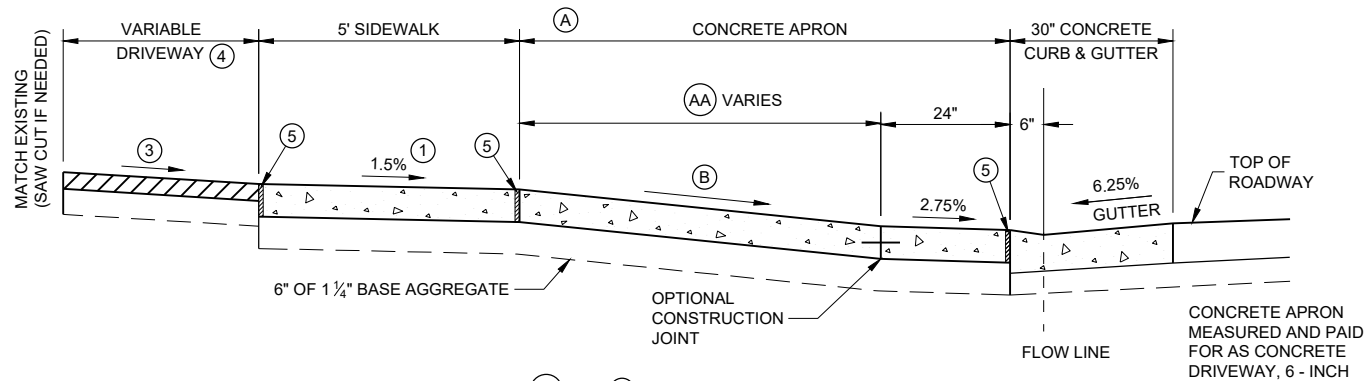
GENERAL NOTES

- PROVIDE CONSTRUCTION JOINTS ALONG THE CENTER OF THE CONCRETE FOR DRIVEWAYS UNDER 20 FEET IN WIDTH AND AT THE THIRD POINTS OVER 20 FEET IN WIDTH.
- (W) IS SHOWN ON PLAN AND PROFILE SHEETS.
- OFFSETS, ELEVATIONS, AND PERCENT GRADE ARE SHOWN ON THE CROSS SECTIONS.
- CONSTRUCTION TOLERANCE OF 0.5%± FOR SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.
 - THE SIDEWALK RAMP MAXIMUM RUNNING SLOPE SHALL NOT REQUIRE THE RAMP LENGTH TO EXCEED 15 FEET TO AVOID CHASING THE SLOPE INDEFINITELY WHEN CONNECTING TO STEEP GRADES. WHEN APPLYING THE 15 FOOT MAXIMUM LENGTH, THE RUNNING SLOPE OF THE SIDEWALK SHALL BE AS FLAT AS FEASIBLE AND NOT EXCEED THE LONGITUDINAL GRADE OF THE ROADWAY.
 - DRIVEWAY SLOPES: DESIRABLE MAXIMUM**
10.5% UP AWAY FROM SIDEWALK (SAG)
8.5% DOWN AWAY FROM SIDEWALK (CREST)
ABSOLUTE MAXIMUM 15% FOR BOTH CREST AND SAG
 - DRIVEWAY TYPES**
· 6-INCH CONCRETE DRIVEWAY PAVEMENT OVER 6-INCH BASE AGGREGATE
· 2-INCH TO 3-INCH ASPHALTIC SURFACE OVER 6-INCH BASE AGGREGATE
· 6-INCH BASE AGGREGATE (MAY BE INCREASED FOR CLAY SUBGRADES.)
 - ½" EXPANSION JOINT FILLER.

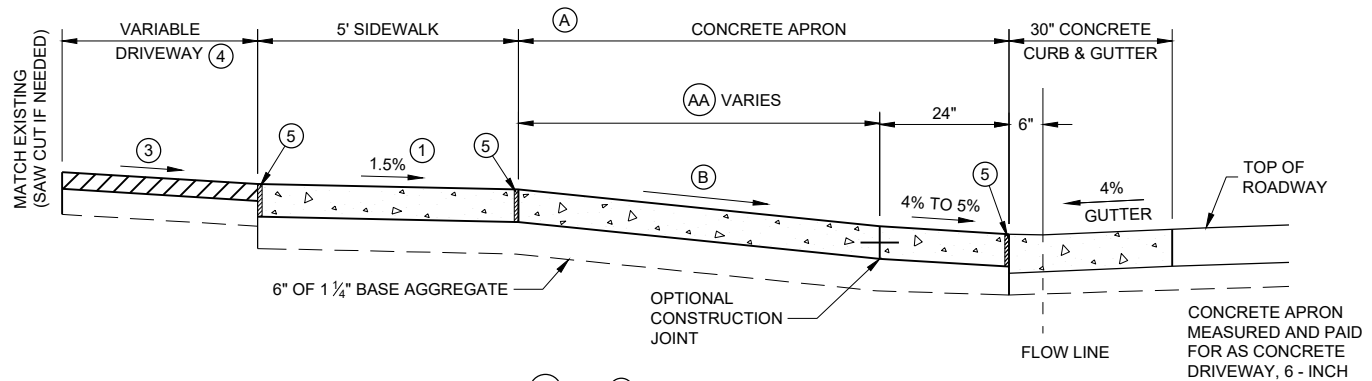
TABLE Z

(AA) FEET	(B) % 6.25% GUTTER	(B) % 4% GUTTER
4.5'	11.5%	9% TO 11.5%
5.5'	9% TO 11.5%	8% TO 11.5%
6.5'	8% TO 11.5%	6% TO 11.5%
7.5'	7% TO 11.5%	6% TO 11.5%
8.5'	6% TO 11.5%	5% TO 11.5%
9.5'	5% TO 11.5%	4% TO 11.5%

(W): 12' MIN. - 24' MAX. RESIDENTIAL AND NON-COMMERCIAL (PE & FE)
16' MIN. - 35' MAX. COMMERCIAL (CE)



6.25% GUTTER SLOPE



4% GUTTER SLOPE

NOTE: SIDEWALK MAY BE DEPRESSED IN DRIVEWAY AREAS FOR (B) VALUES NOT SHOWN IN TABLE Z.
SIDEWALK WITHIN THE LIMITS OF THE DRIVEWAY PAID FOR AS CONCRETE DRIVEWAY 6-INCH.
SEPARATE PAYMENT FOR BASE AGGREGATE WILL BE MADE.

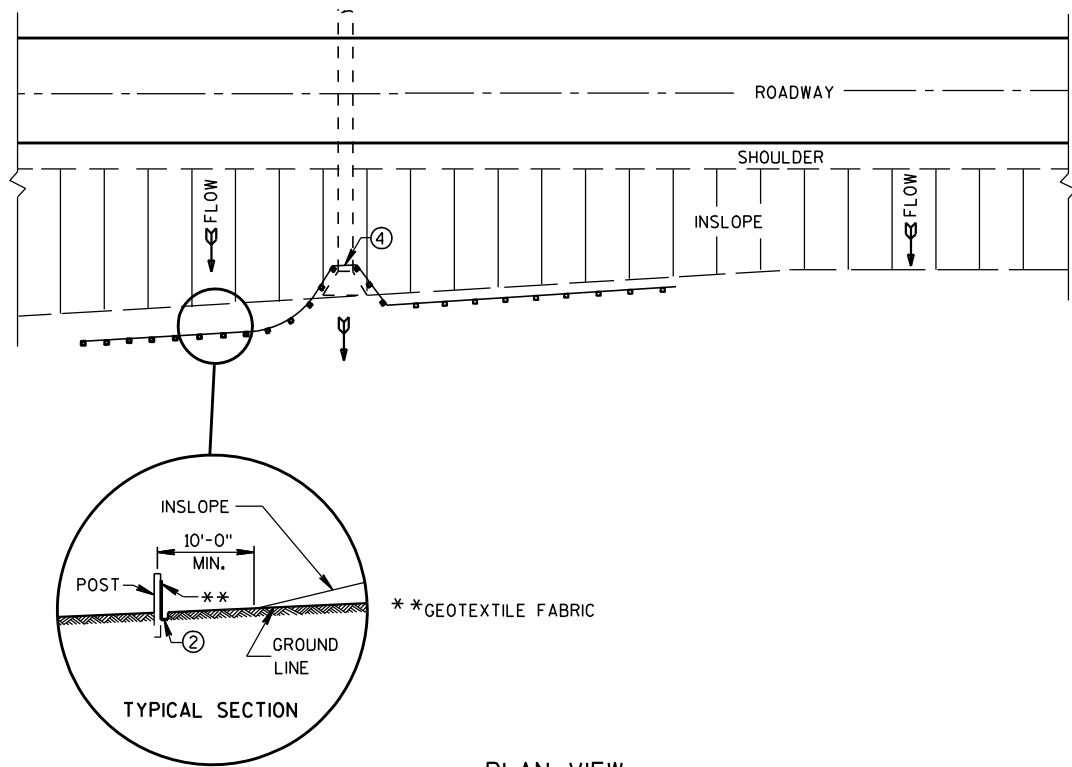
**SECTION Z - Z
DRIVEWAY DETAIL WITH CONCRETE CURB AND GUTTER
(URBAN AND SUBURBAN)**

DRIVEWAY AND SIDEWALK RAMPS TYPE Z

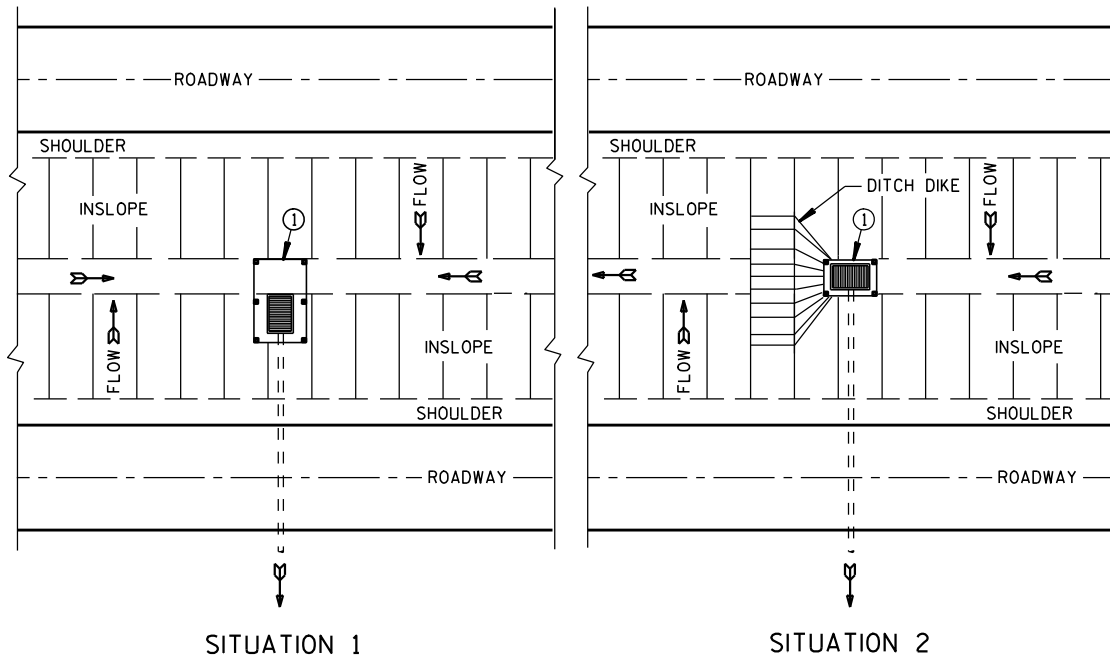
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2022 DATE /S/ Rodney Taylor
ROADWAY STANDARDS DEVELOPMENT ENGINEER

FHWA



PLAN VIEW
TYPICAL APPLICATION OF SILT FENCE

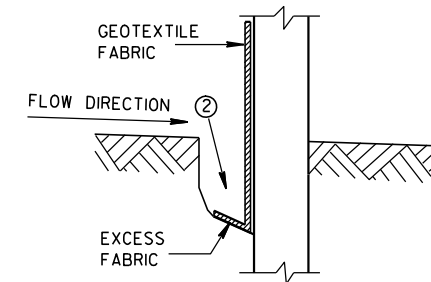


SITUATION 1 SITUATION 2
PLAN VIEW
SILT FENCE AT MEDIAN SURFACE DRAINS

GENERAL NOTES

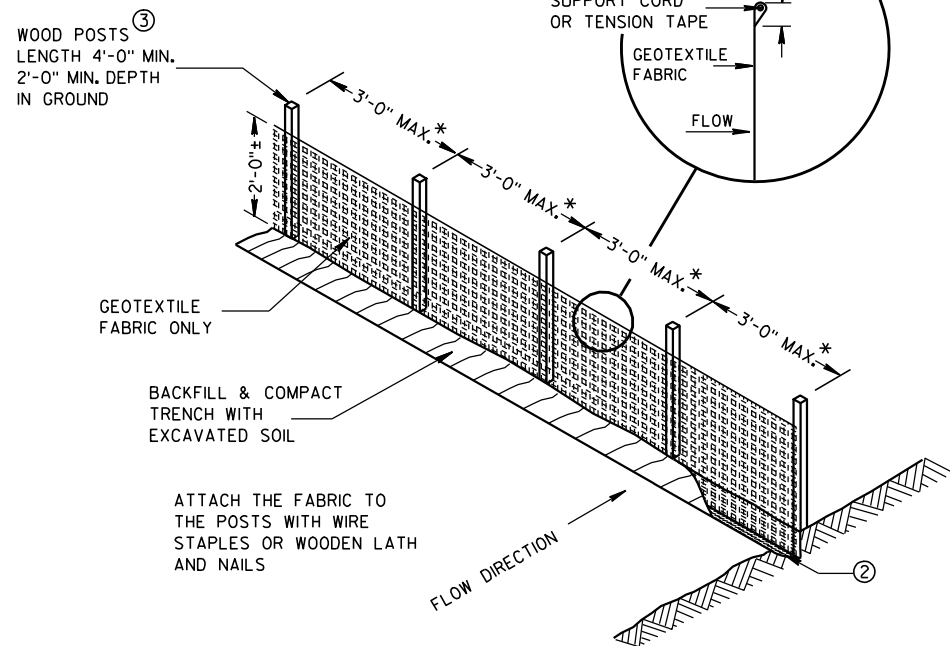
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

- ① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- ③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1 1/8" X 1 1/8" OF OAK OR HICKORY.
- ④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- ⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.

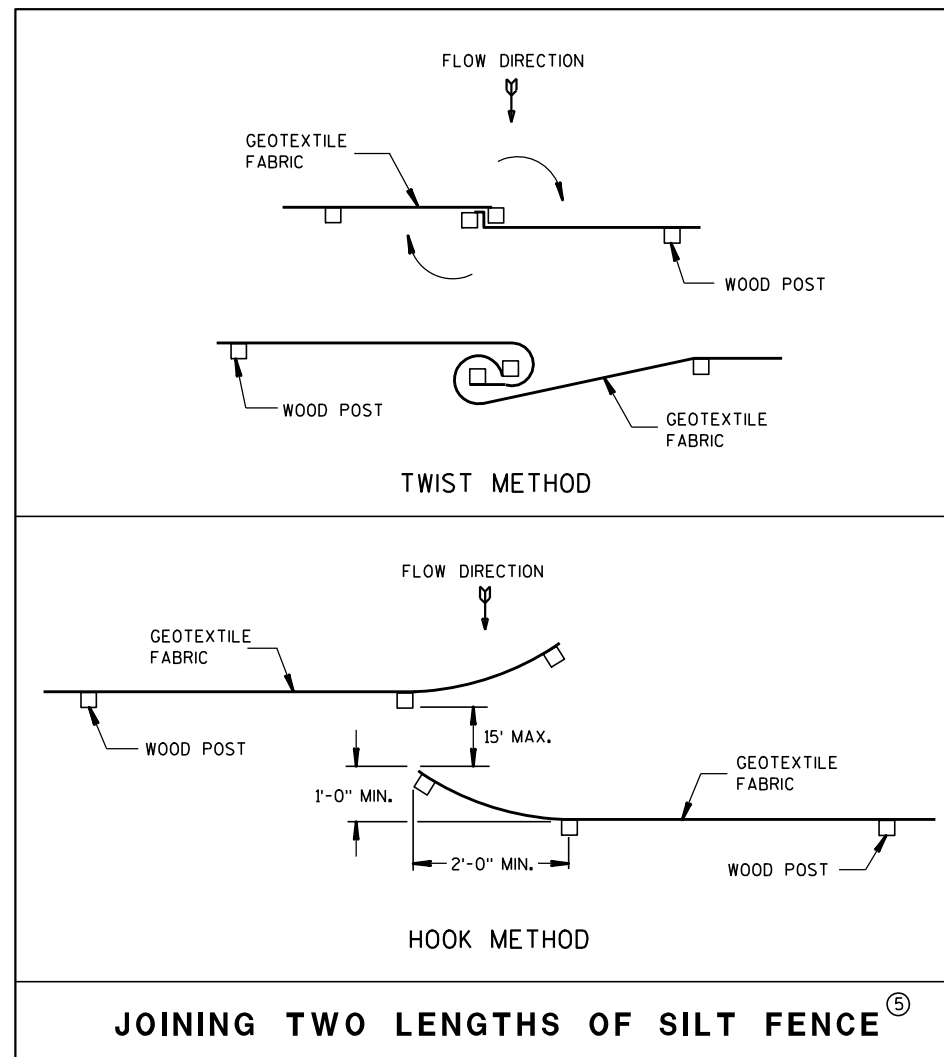


TRENCH DETAIL

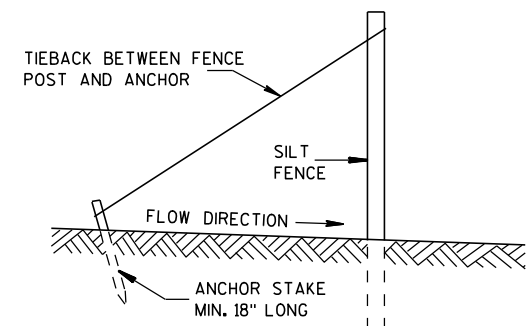
NOTE: ADDITIONAL POST DEPTH OR TIE BACKS MAY BE REQUIRED IN UNSTABLE SOILS



SILT FENCE



JOINING TWO LENGTHS OF SILT FENCE ⑤

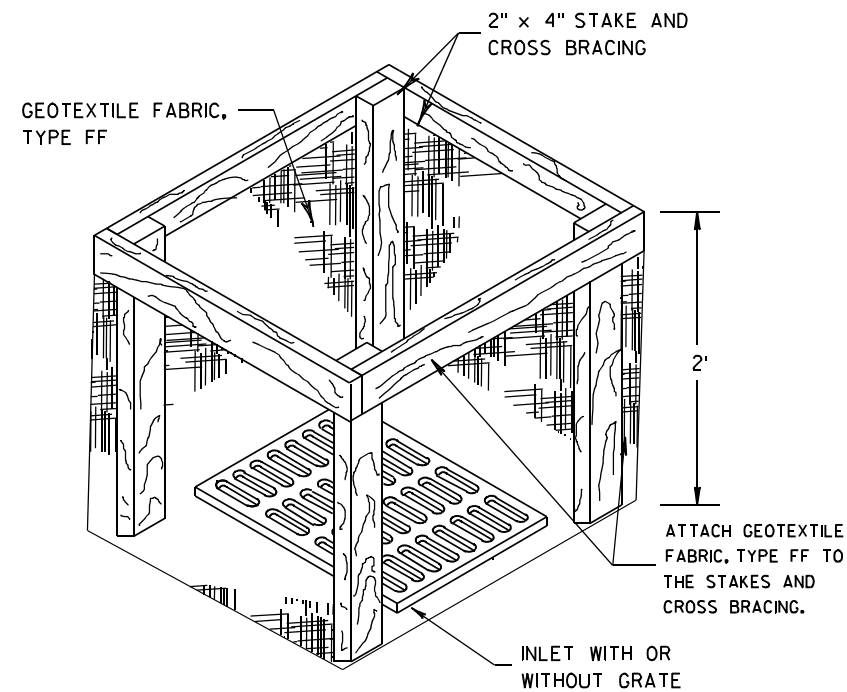
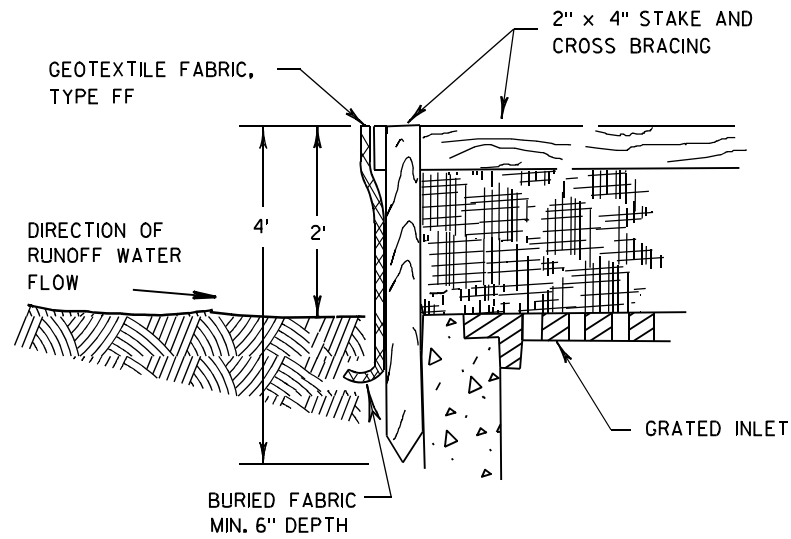


SILT FENCE TIE BACK
(WHEN REQUIRED BY THE ENGINEER)

SILT FENCE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
4-29-05 /S/ Beth Cannestra
DATE CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA



INLET PROTECTION, TYPE A

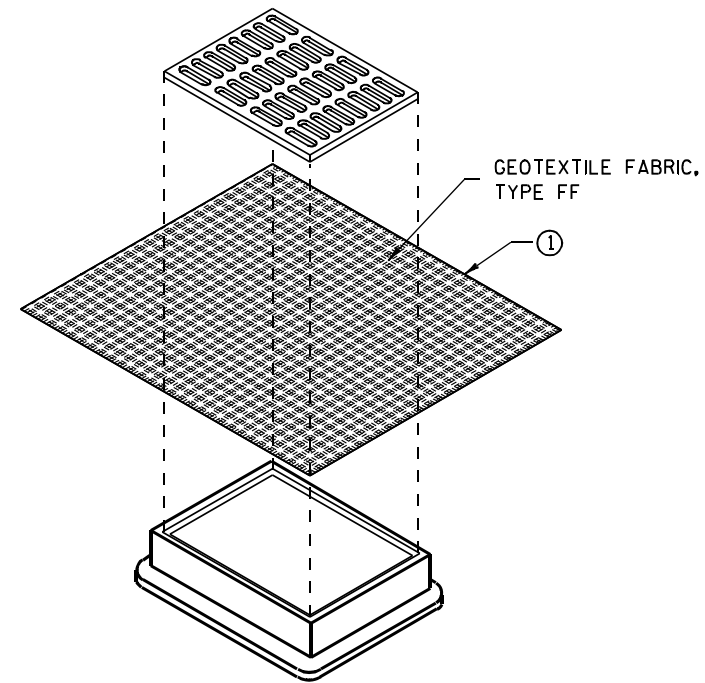
GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

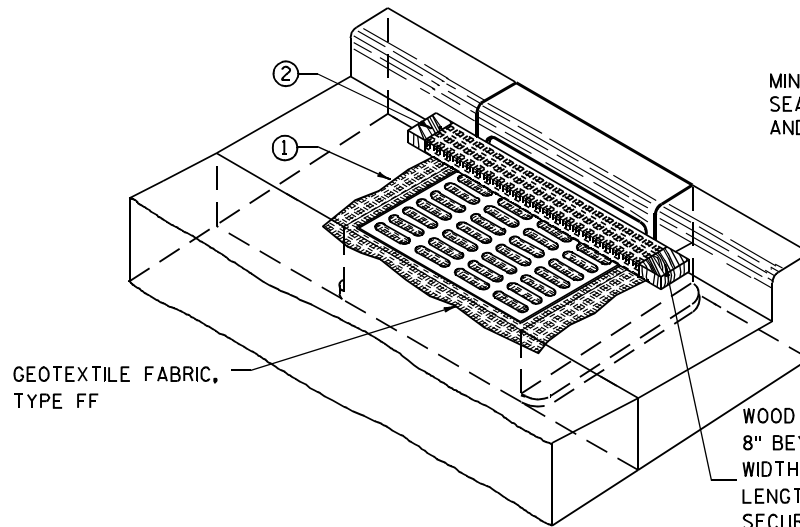
WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- ① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- ② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- ③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



**INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)**

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE C (WITH CURB BOX)

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

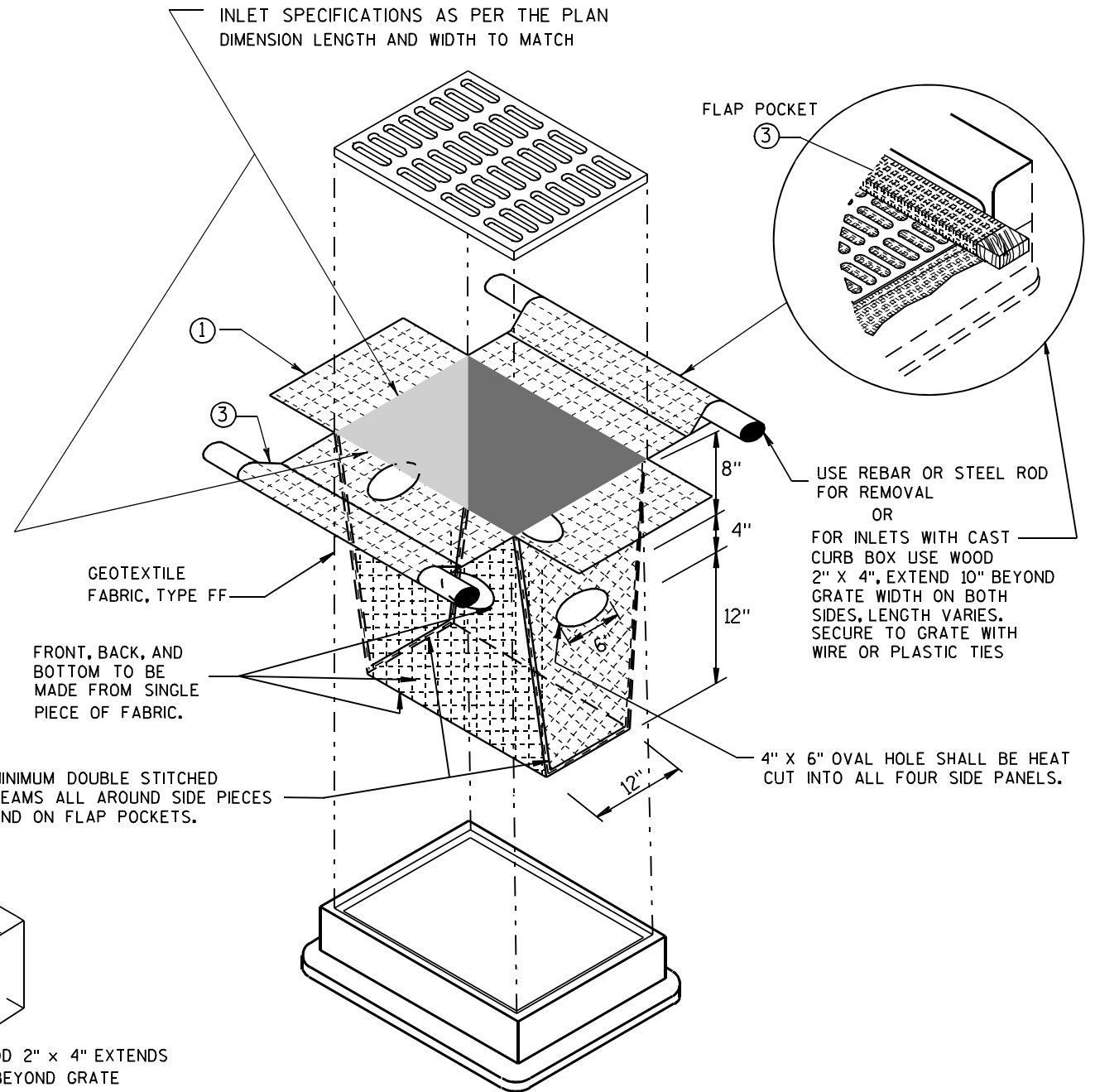
THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.



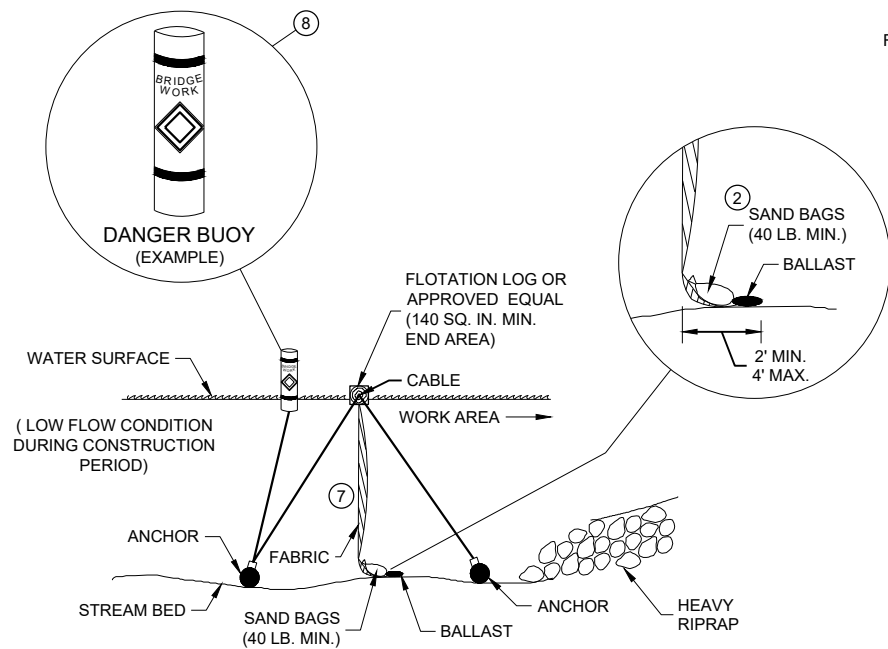
INLET PROTECTION, TYPE D

(CAN BE INSTALLED IN ANY INLET TYPE WITH OR WITHOUT A CURB BOX AS PER NOTE ②)

**INLET PROTECTION
TYPE A, B, C, AND D**

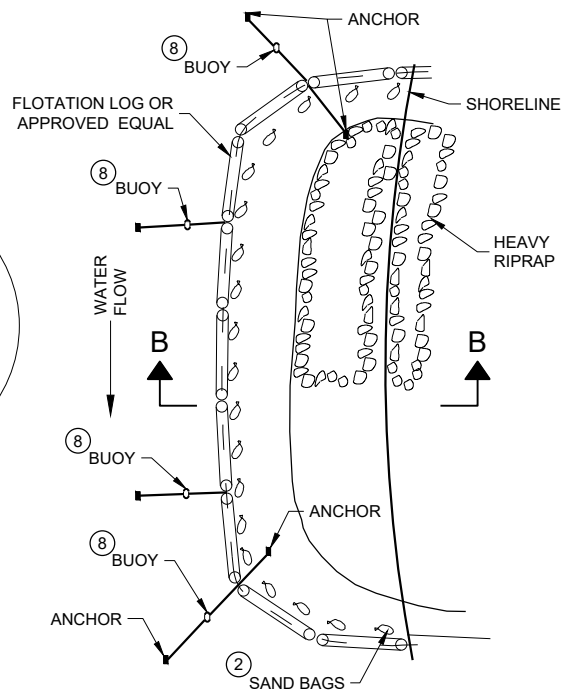
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
10/16/02 /S/ Beth Connestra
DATE
CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA

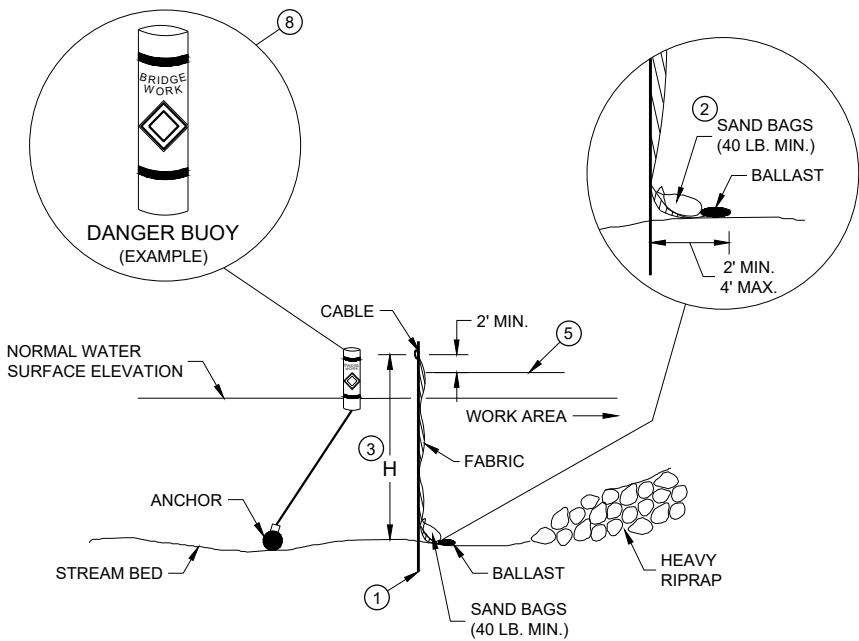


SECTION B - B

**TURBIDITY BARRIER - FLOAT ALTERNATIVE
CAUTION - SEE NOTE 6**

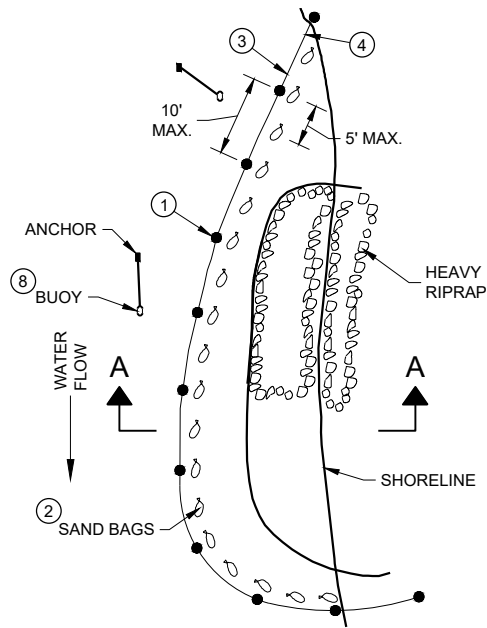


PLAN VIEW



SECTION A - A

TURBIDITY BARRIER - STANDARD POST INSTALLATION



PLAN VIEW

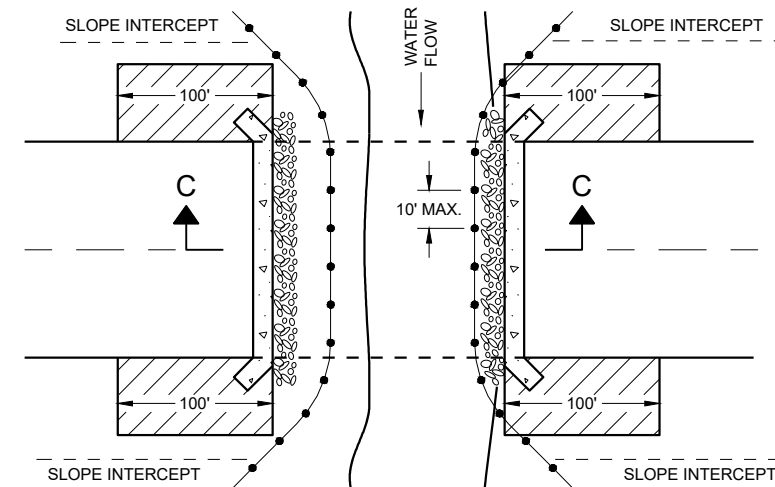
TURBIDITY BARRIER PLACEMENT DETAILS

GENERAL NOTES

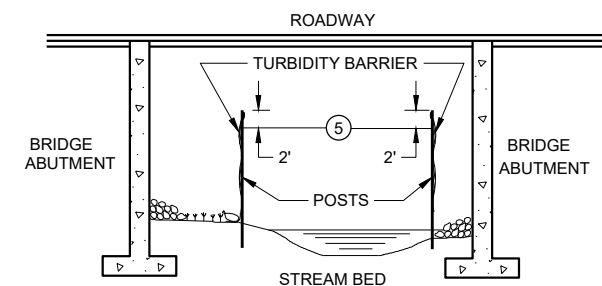
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

TURBIDITY BARRIER MAY BE REMOVED AT THE ENGINEERS DISCRETION, WHEN PERMANENT EROSION CONTROL MEASURES HAVE BEEN ESTABLISHED.

- ① DRIVEN STEEL POSTS, PIPES, OR CHANNELS. LENGTH SHALL BE SUFFICIENT TO SECURELY SUPPORT BARRIER AT HIGH WATER ELEVATIONS.
- ② SAND BAGS TO BE USED AS ADDITIONAL BALLAST WHEN ORDERED BY THE ENGINEER TO MEET ADVERSE FIELD CONDITIONS. SPACE AS APPROPRIATE FOR SITE CONDITIONS.
- ③ WHEN BARRIER HEIGHT "H" EXCEEDS 8 FEET, POST SPACING MAY NEED TO BE DECREASED.
- ④ IN WATERWAYS SUBJECT TO FLUCTUATING WATER ELEVATIONS, PROVISIONS SHOULD BE MADE TO ALLOW THE WATER TO EQUALIZE ON EACH SIDE OF THE BARRIER. THIS MAY BE ACCOMPLISHED BY LEAVING A PORTION OF THE BARRIER OPEN ON THE UPSTREAM END.
- ⑤ ESTIMATED HIGH WATER ELEVATION DURING CONSTRUCTION PERIOD. MINIMUM BARRIER HEIGHT SHALL BE 2' GREATER THAN EITHER THE Q2 ELEVATION OR THE ESTIMATED HIGH WATER ELEVATION DURING CONSTRUCTION, WHICHEVER IS GREATER.
- ⑥ FLOAT ALTERNATIVE WILL ONLY BE ALLOWED WITH WRITTEN APPROVAL OF THE ENGINEER, AND IS MEANT FOR LOCATIONS WHERE BEDROCK PREVENTS THE INSTALLATION OF POSTS.
- ⑦ ALLOW SUFFICIENT SLACK VERTICALLY AND HORIZONTALLY SO THAT SEDIMENT BUILD UP WILL NOT SEPARATE OR LOWER THE TURBIDITY BARRIER.
- ⑧ USE AS DIRECTED BY COAST GUARD OR DNR PERMIT WHEN WORKING IN NAVIGABLE WATERWAYS.



PLAN VIEW



SECTION C - C

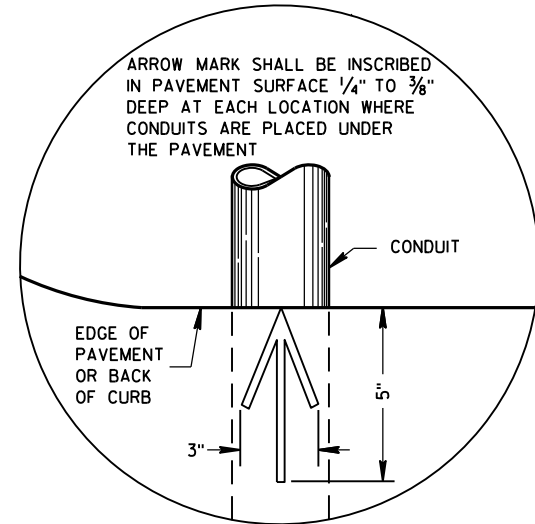
**TURBIDITY BARRIER DETAIL SHOWING
TYPICAL PLACEMENT AT STRUCTURES**

TURBIDITY BARRIER

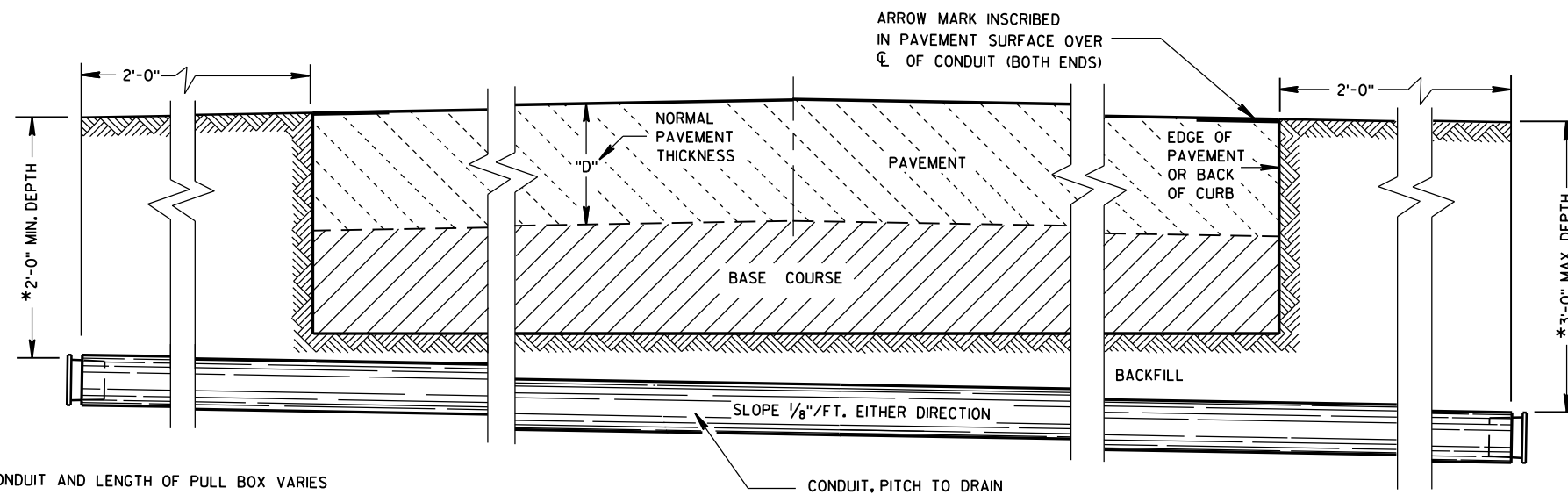
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
6/4/02 DATE /S/ Beth Cannestra
DATE CHIEF ROADWAY DEVELOPMENT
ENGINEER

FHWA



**PLAN VIEW
ARROW MARK**



**SIDE ELEVATION
DETAIL FOR CONDUIT UNDER PAVED HIGHWAYS**

*DEPTH OF CONDUIT AND LENGTH OF PULL BOX VARIES WITH HEIGHT OF CURB USED. ALSO SEE PULL BOX S.D.D. 9B4

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

METALLIC (STANDARD SPECIFICATION 652.2.2) OR NONMETALLIC (STANDARD SPECIFICATION 652.2.3) CONDUIT SHALL BE FURNISHED AND PLACED AS SHOWN.

DEPTH OF CONDUIT INSTALLED BELOW THE TRAVELED WAY SHALL BE 24 INCHES MINIMUM AND 36 INCHES MAXIMUM.

DEPTH OF CONDUIT INSTALLED THAT IS NOT BELOW THE TRAVELED WAY SHALL BE 18 INCHES MINIMUM AND 36 INCHES MAXIMUM.

ANY EXCEPTION TO THE MAXIMUM DEPTH SHALL BE ONLY WITH THE WRITTEN APPROVAL OF THE ENGINEER.

THE TRENCH SHALL NOT BE BACKFILLED PRIOR TO INSPECTION OF THE CONDUIT.

ALL METALLIC CONDUIT RACEWAY ENDS SHALL BE REAMED AND THREADED.

ALL METALLIC CONDUIT IN WHICH WIRE OR CABLE IS TO BE INSTALLED SHALL BE BUSHED WITH APPROVED THREADED BUSHINGS BEFORE INSTALLATION OF THE WIRE OR CABLE.

ALL METALLIC CONDUITS IN WHICH WIRE OR CABLE IS NOT TO BE INSTALLED SHALL BE CAPPED WITH THREADED PROTECTIVE CAPS, AS APPROVED BY THE ENGINEER.

ALL NONMETALLIC CONDUIT SHALL BE CAPPED OR PLUGGED IMMEDIATELY AFTER INSTALLATION AND SHALL REMAIN CAPPED OR PLUGGED UNTIL WIRE/CABLES ARE INSTALLED.

NONMETALLIC CONDUITS IN WHICH WIRE OR CABLE IS NOT BEING INSTALLED SHALL REMAIN CAPPED OR PLUGGED.

BENDING OF PVC ELECTRICAL CONDUIT SHALL BE ACCOMPLISHED BY USING A BLANKET OR EMERSON TYPE TANK DESIGNED FOR THE PURPOSE OF BENDING PVC ELECTRICAL CONDUIT.

ALL CUT ENDS SHALL BE TRIMMED INSIDE AND OUTSIDE TO REMOVE ALL ROUGH EDGES ON NONMETALLIC CONDUIT. (SEE NEC 347.5)

WHEN REQUIRED TO CONNECT NONMETALLIC CONDUIT TO METALLIC CONDUIT, ONLY U.L. LISTED ADAPTER FITTINGS SHALL BE USED.

PRIOR TO CONDUIT ACCEPTANCE, CONDUIT CAPS OR PLUGS SHALL BE REMOVED, AND THE CAPS, PLUGS AND CONDUIT ENDS SHALL BE THOROUGHLY CLEANED AND THEN THE CAPS OR PLUGS REINSTALLED TO ENSURE THAT THE CAPS OR PLUGS CAN BE EASILY REMOVED IN THE FUTURE.

ALL CONDUIT BEING FURNISHED AND INSTALLED SHALL HAVE THE U.L. LABEL FIRMLY ATTACHED.

CONDUIT RUNS SHALL BE THE SAME SIZE OF CONDUIT FROM ONE END TO THE OTHER (FROM PULL BOX TO PULL BOX-OR-JUNCTION BOX TO JUNCTION BOX-OR-BASE TO BASE, ETC.).

TRACER WIRE SHALL BE INSTALLED AS STATED IN THE STANDARD SPECIFICATION, ITEM 652.3.1.1.

ALL CONDUIT RUNS SHALL BE STRAIGHT (WITHOUT BENDS) FROM PULL BOX TO PULL BOX, PULL BOX TO BASE AND BASE TO BASE AS SHOWN ON THE PLANS.

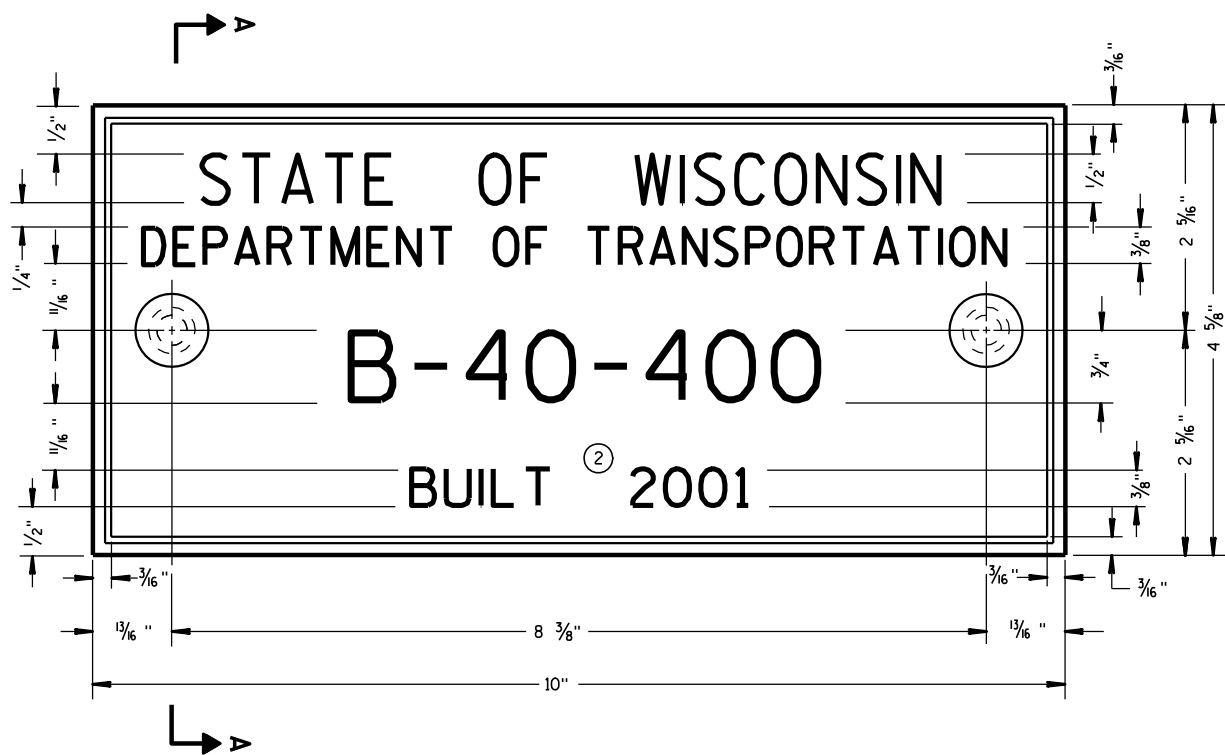
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6

S.D.D. 9 B 2-10

S.D.D. 9 B 2-10

CONDUIT	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED March, 2017 DATE	/S/ Ahmet Demirbilek STATE ELECTRICAL ENGINEER
FHWA	



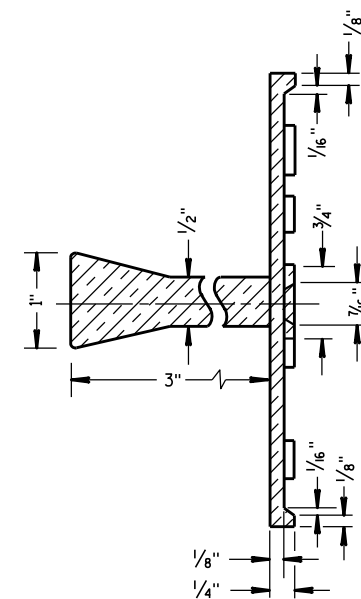
TYPICAL NAME PLATE
(BRIDGES, CULVERTS, AND RETAINING WALLS)

GENERAL NOTES

NAME PLATES TO BE INSTALLED ON BRIDGES, CULVERTS, AND RETAINING WALLS SHALL CONFORM TO THE REQUIREMENTS OF SECTION 502.3.11 OF THE STANDARD SPECIFICATIONS.

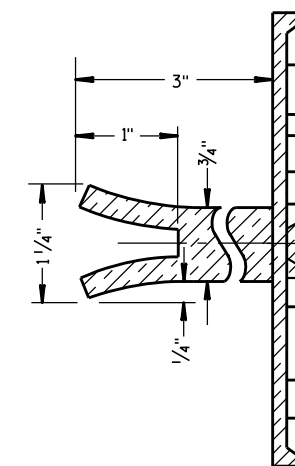
THE BRIDGE NUMBER AND YEAR BUILT SHOWN ON THIS DRAWING ARE EXAMPLES ONLY. SEE CONSTRUCTION PLANS FOR INDIVIDUAL NUMBERING AND YEAR BUILT.

- ① EPOXY RESIN SHALL BE FROM AN APPROVED MANUFACTURER AND USED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS.
- ② REHABILITATION OF AN EXISTING STRUCTURE SHOULD USE THE DATE OF ORIGINAL STRUCTURE CONSTRUCTION.

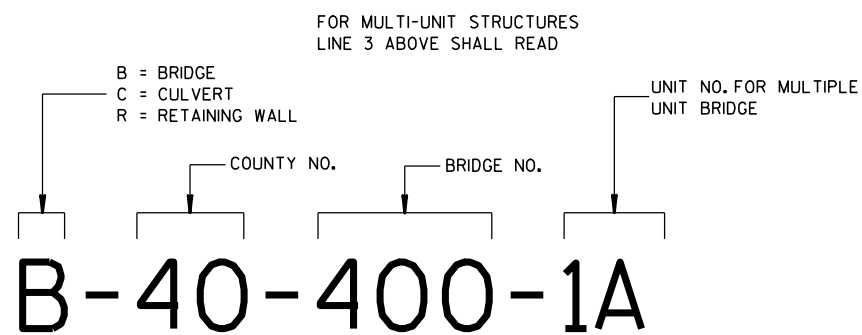


SECTION A-A

SPREAD OPEN SO THE TOP OF LUG IS 1 1/4" WIDE

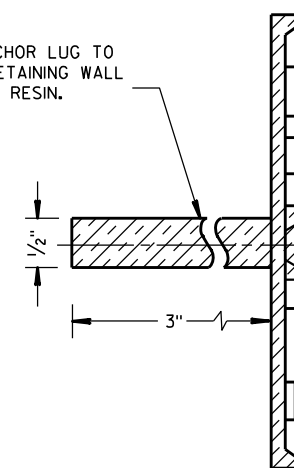


ALTERNATE LUG



**NUMBERING DESIGNATION
MULTI-UNIT STRUCTURES**

- ① ADHERE ANCHOR LUG TO PRECAST RETAINING WALL WITH EPOXY RESIN.

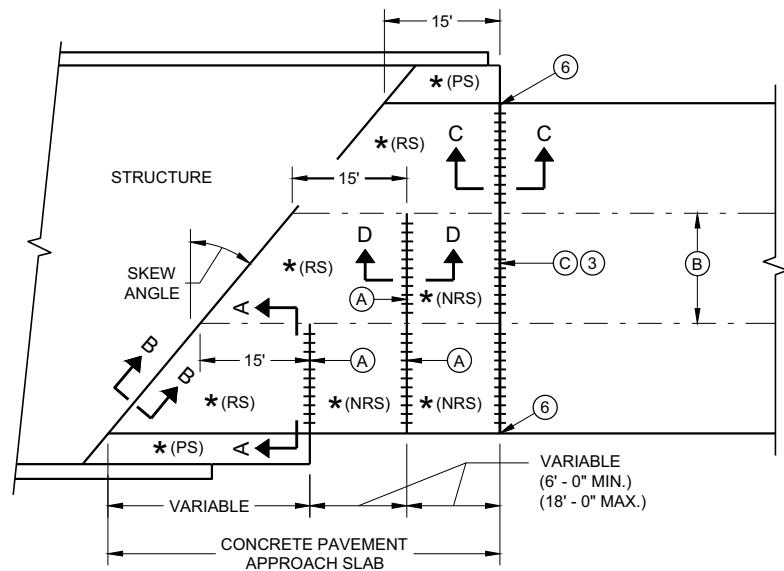


ALTERNATE LUG
(FOR ATTACHMENT TO PRECAST STRUCTURES)

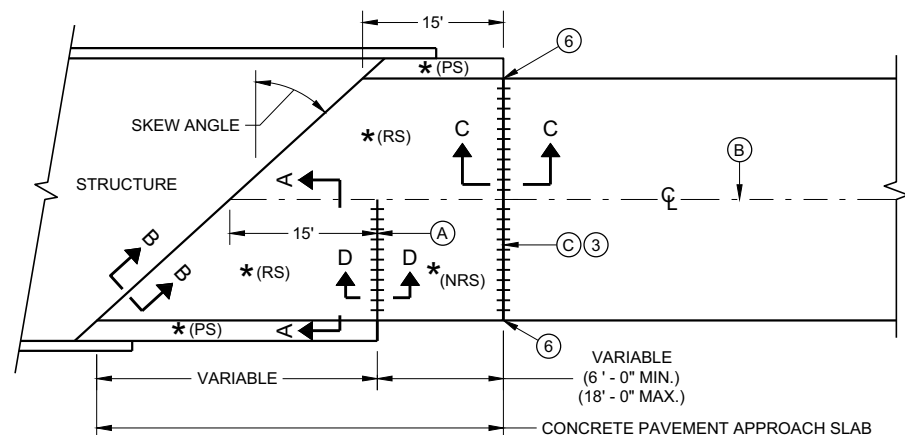
**NAME PLATE
(STRUCTURES)**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

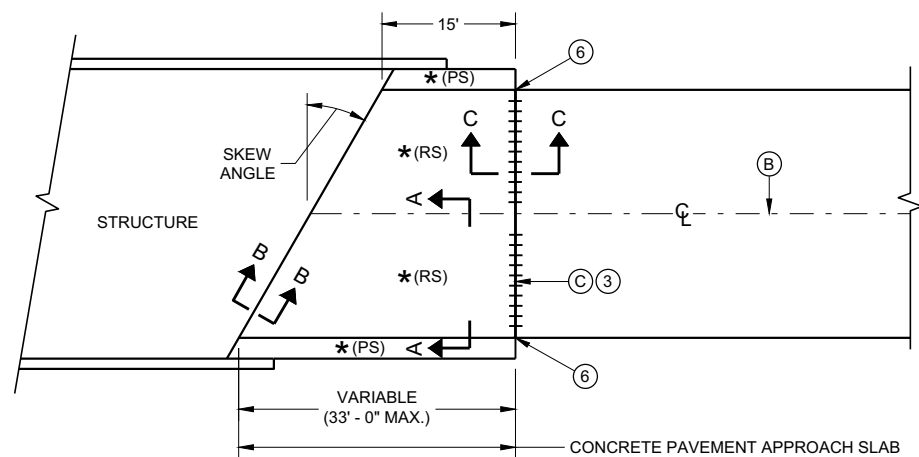
APPROVED
DATE 3/26/10 /S/ Scot Becker
CHIEF STRUCTURAL DEVELOPMENT ENGINEER
FHWA



**SKewed APPROACH
(PAVEMENT MORE THAN TWO LANES)**

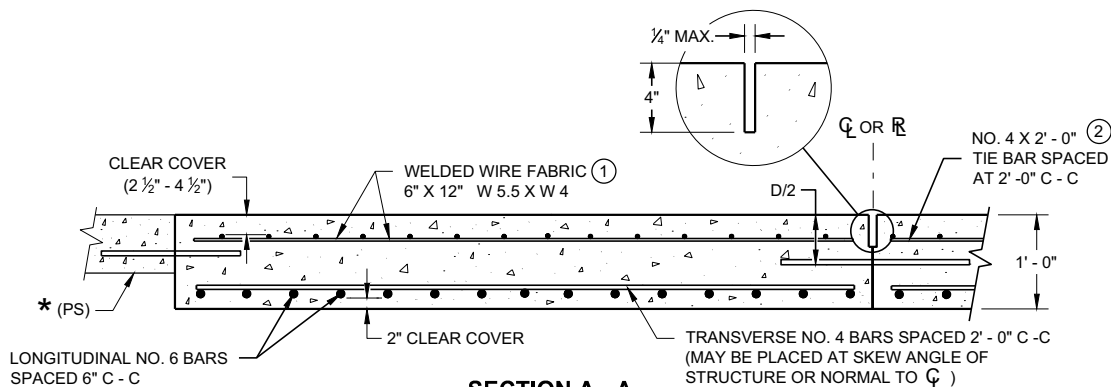


**SKews > 20°
(PAVEMENT WIDTH ≤ 30')**

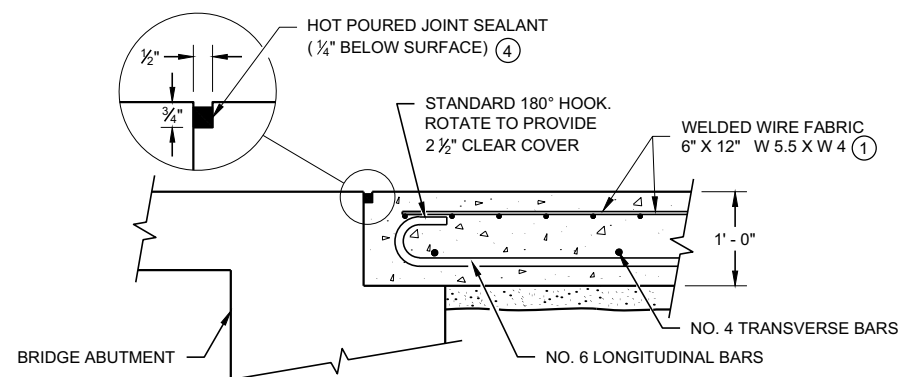


**SKews ≤ 20°
(PAVEMENT WIDTH ≤ 30')**
APPROACH SLAB AND ADJACENT PAVEMENT

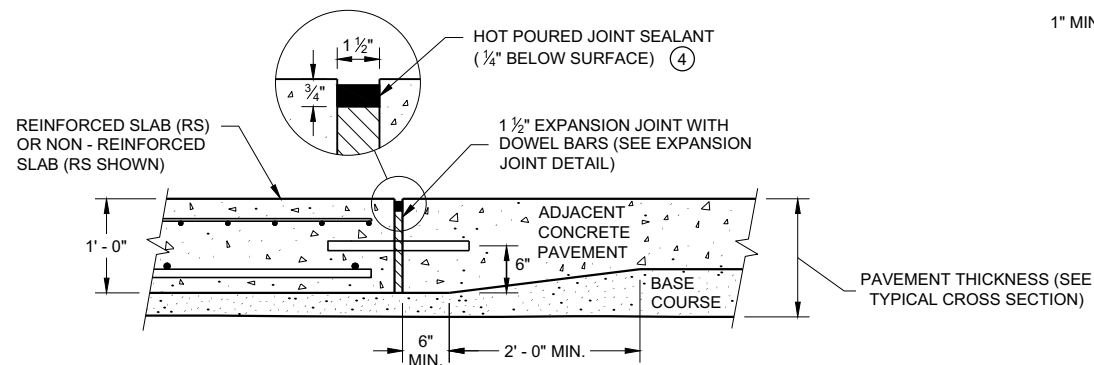
- * (RS) = REINFORCED CONCRETE SLAB
- * (PS) = PAVED CONCRETE SHOULDER OR CONCRETE DRAINAGE SLAB
- * (NRS) = NON - REINFORCED CONCRETE SLAB
- *** STANDARD DOWEL BAR DIAMETER (SEE SDD 13C11 AND SDD 13C13)



**SECTION A - A
REINFORCEMENT POSITIONING DETAIL**



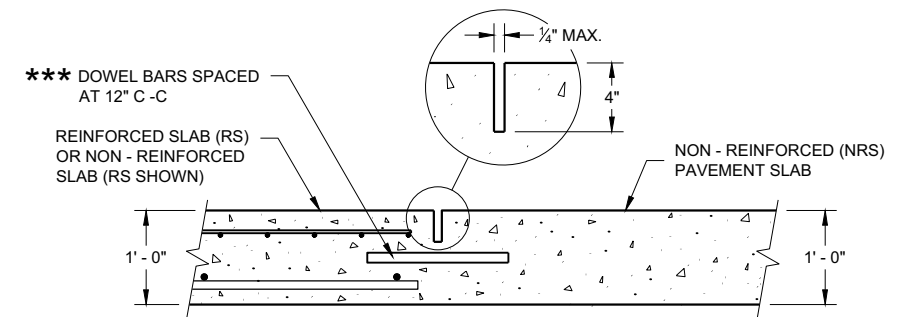
**SECTION B - B
BEND DETAIL
BOTTOM REINFORCEMENT**



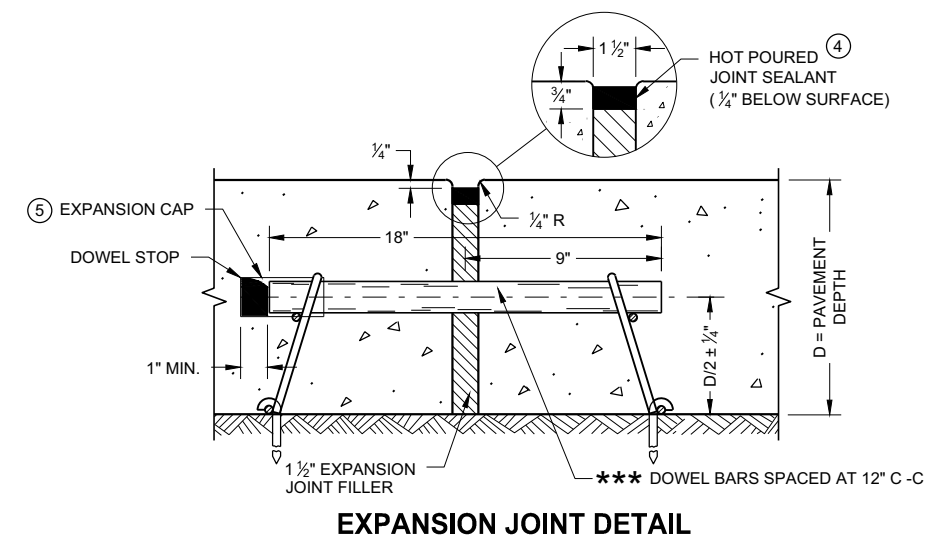
**SECTION C - C
TRANSITION DETAIL
APPROACH SLAB TO ADJACENT PAVEMENT**

GENERAL NOTES

- THE CONTRACTOR MAY SPLICE NO. 6 BARS IN THE APPROACH SLAB FOR SKEWED STRUCTURES ONLY. STAGGER SPLICES WITH A MAXIMUM OF ONE SPLICE PER BAR. THE LENGTH OF LAP IS 20 INCHES.
- TACK WELD DOWEL BARS TO THE BASKETS ON ALTERNATE ENDS.
- ① THE CONTRACTOR MAY USE NO. 4 BARS SPACED AT 2' - 0" C - C IN BOTH THE LONGITUDINAL AND TRANSVERSE DIRECTIONS FOR TOP REINFORCEMENT AS AN ALTERNATIVE TO THE WELDED WIRE FABRIC.
 - ② THE CONTRACTOR MAY OMIT THE BARS BETWEEN REINFORCED SLABS WHERE SLAB REINFORCEMENT BARS EXTEND ACROSS THE CENTERLINE OR REFERENCE LINE.
 - ③ DO NOT CONSTRUCT AN EXPANSION JOINT OR INSTALL DOWEL BARS WHEN ABUTTING AN HMA PAVEMENT.
 - ④ USE A JOINT SEALANT CONFORMING TO STANDARD SPECIFICATION 415.2.6.
 - ⑤ PLACE EXPANSION CAP ON THE END OF THE DOWEL THAT IS NOT TACK WELDED TO THE BASKET. DO NOT FORCE DOWEL BAR PAST THE DOWEL STOP.
 - ⑥ EXTEND EXPANSION JOINT THROUGH ANY ADJACENT TIED CONCRETE.
 - (A) STANDARD CONTRACTION JOINT NORMAL TO \bar{C} OR \bar{R} .
 - (B) STANDARD LONGITUDINAL JOINT WITH TIE BARS.
 - (C) 1 1/2" EXPANSION JOINT WITH DOWEL BARS NORMAL TO \bar{C} OR \bar{R} .



**SECTION D - D
CONTRACTION JOINT**



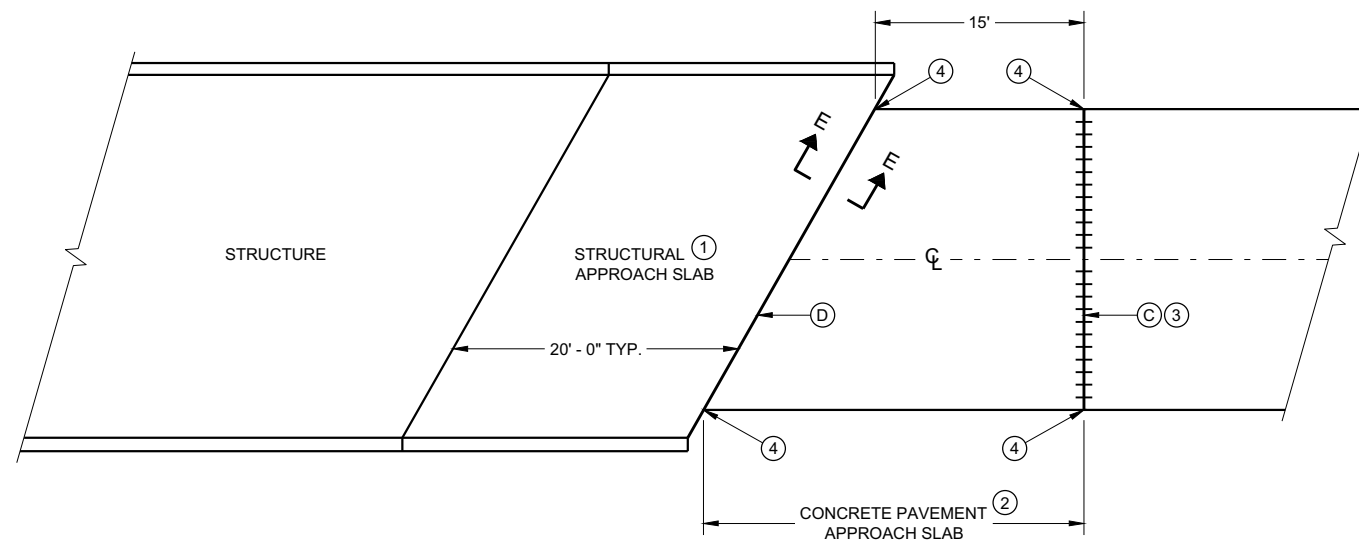
EXPANSION JOINT DETAIL

**CONCRETE PAVEMENT
APPROACH SLAB**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2018 /S/ Peter Kemp, P.E.
DATE DATE PAVEMENT SUPERVISOR

FHWA

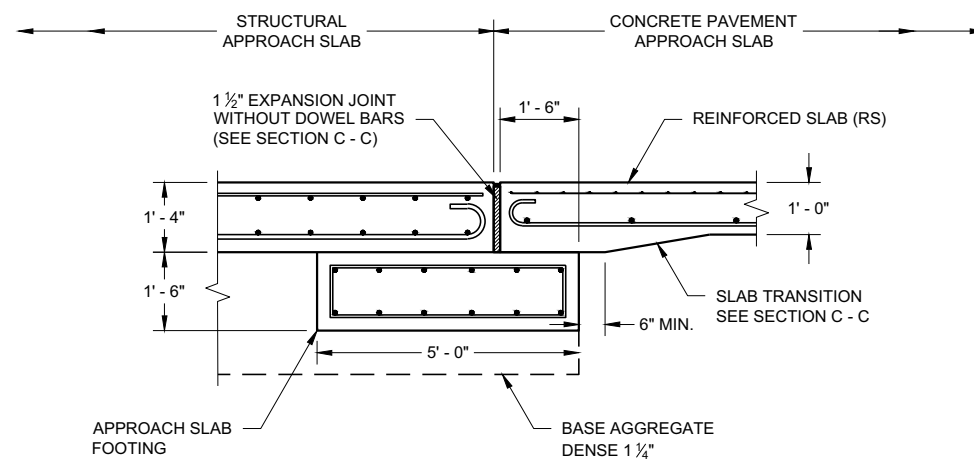


GENERAL NOTES

ALL PROJECTS THAT INVOLVE A STRUCTURAL APPROACH SLAB WILL ALSO HAVE A CONCRETE PAVEMENT APPROACH SLAB.

- ① SEE BRIDGE PLAN.
- ② CONFORM TO SDD 13B02 SHEET A FOR CONCRETE PAVEMENT APPROACH SLAB DETAILS
- ③ DO NOT CONSTRUCT AN EXPANSION JOINT OR INSTALL DOWEL BARS WHEN ABUTTING AN HMA PAVEMENT.
- ④ EXTEND EXPANSION JOINT THROUGH ANY ADJACENT TIED CONCRETE.
- Ⓒ 1½" EXPANSION JOINT WITH DOWEL BARS NORMAL TO CL OR RL .
- Ⓓ 1½" EXPANSION JOINT (NO DOWELS)

BRIDGE APPROACHES

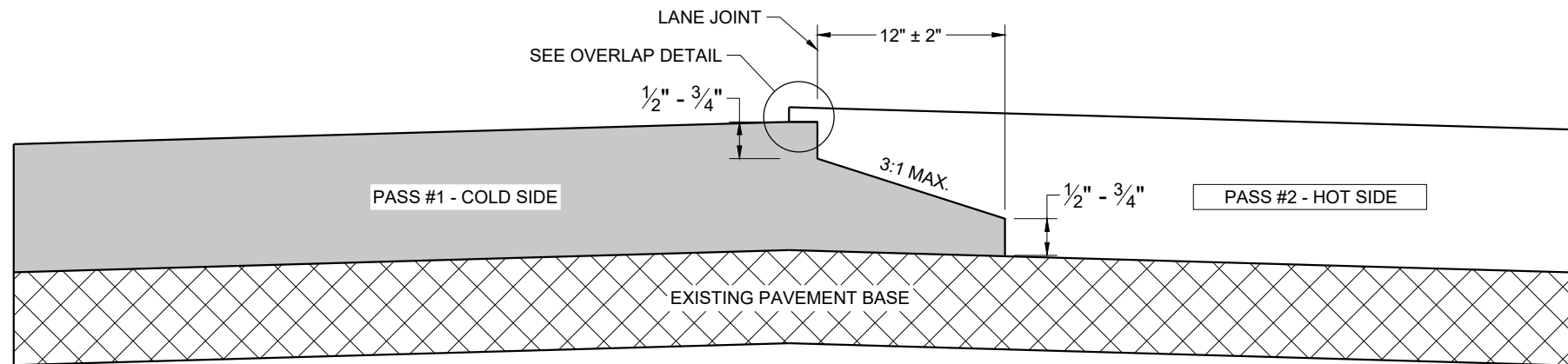


SECTION E - E
FOOTING DETAIL
STRUCTURAL APPROACH SLAB TO CONCRETE BRIDGE APPROACH

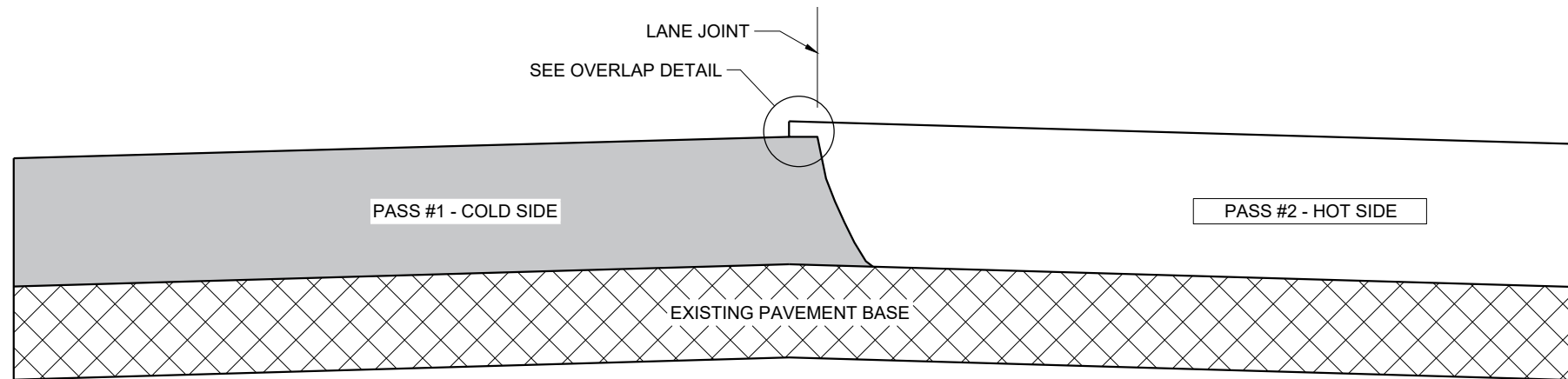
STRUCTURAL APPROACH SLAB AND CONCRETE PAVEMENT APPROACH SLAB

STATE OF WISCONSIN
 DEPARTMENT OF TRANSPORTATION

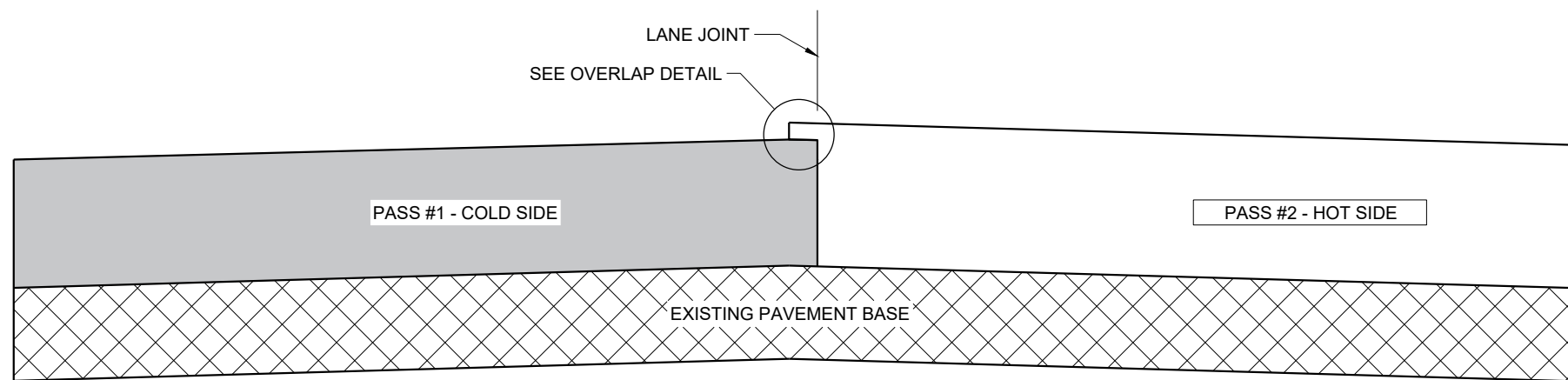
APPROVED
 November 2018 /S/ Peter Kemp P.E.
 DATE PAVEMENT SUPERVISOR



**TYPICAL PAVEMENT CROSS SECTION
NOTCHED WEDGE JOINT**



**TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT**



**TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT (MILLED)**

GENERAL NOTES

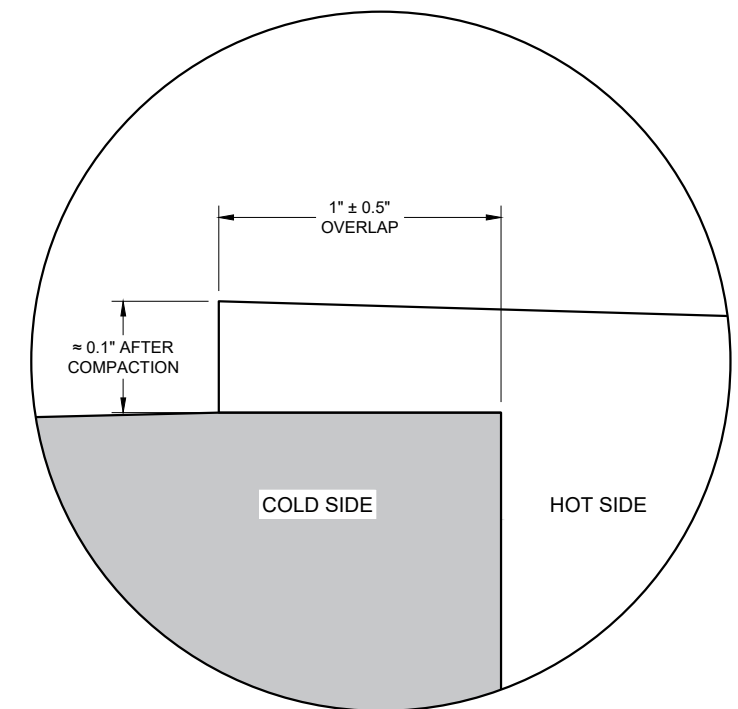
IN ADDITION TO THE DETAILS PROVIDED IN THIS DRAWING, CONFORM TO STANDARD SPECIFICATION 450.3.2.8 FOR WHEN A NOTCHED WEDGE JOINT IS REQUIRED AND FOR GENERAL JOINT CONSTRUCTION REQUIREMENTS.

FOR ALL LONGITUDINAL JOINTS, ENSURE THE PAVER SCREED OVERLAPS THE PREVIOUSLY PLACED PAVEMENT BY $1" \pm 0.5"$ AND THE HOT SIDE OF THE JOINT REMAINS HIGHER THAN THE COLD SIDE BY APPROXIMATELY $0.1"$ AFTER FINAL COMPACTION. (IT WILL BE FLUSH WHEN PAVING IN ECHELON.)

ONLY REMOVE THE LONGITUDINAL NOTCHED WEDGE JOINT FOR SMA PAVEMENT OR AS DIRECTED BY THE ENGINEER TO ADDRESS SPECIFIC LENGTHS OF JOINT DAMAGED BY TRAFFIC.

WHEN MILLING BACK OR REMOVING ANY LONGITUDINAL JOINT, LIMIT THE MATERIAL REMOVED TO $2"$ FROM THE TOP NOTCH OR FROM THE VERTICAL JOINT EDGE ON THE COLD SIDE OF THE JOINT.

USE LONGITUDINAL MILLED JOINT AS PLANS SHOW OR THE AS THE ENGINEER DIRECTS.



OVERLAP DETAIL (TYPICAL)

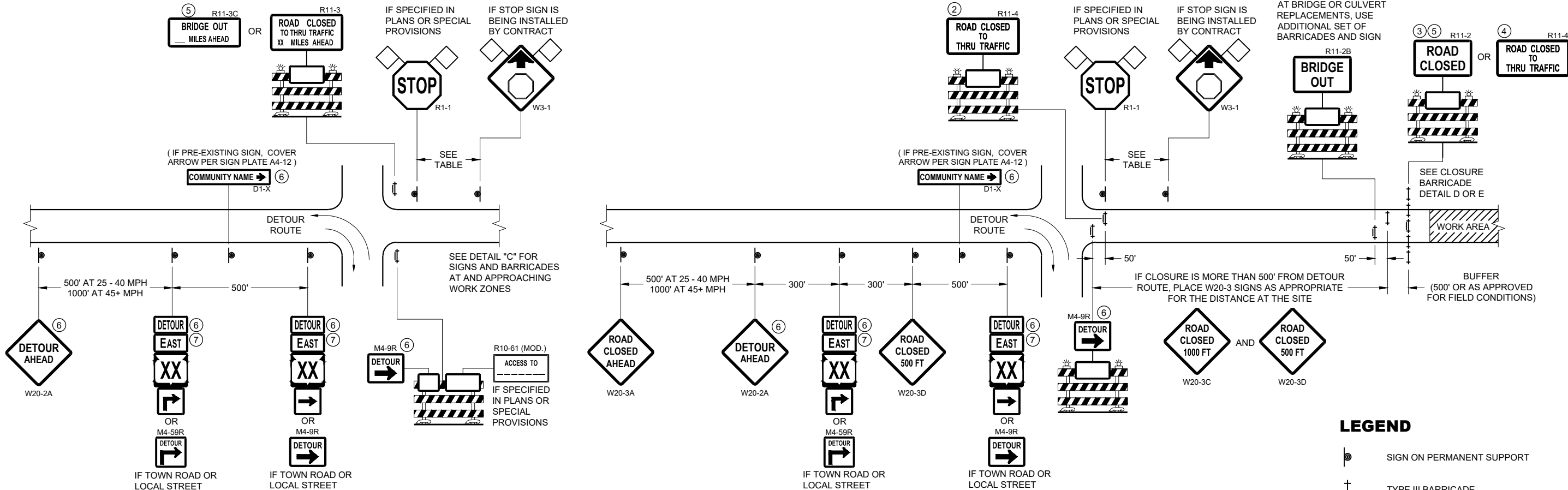
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SDD 13C19 - 03

SDD 13C19 - 03

HMA LONGITUDINAL JOINTS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED November 2020 DATE	/S/ Steven Hefel HMA PAVEMENT ENGINEER
FHWA	



**DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR**

WORK ZONE GREATER THAN OR EQUAL TO 1/2 MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)

**DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR**

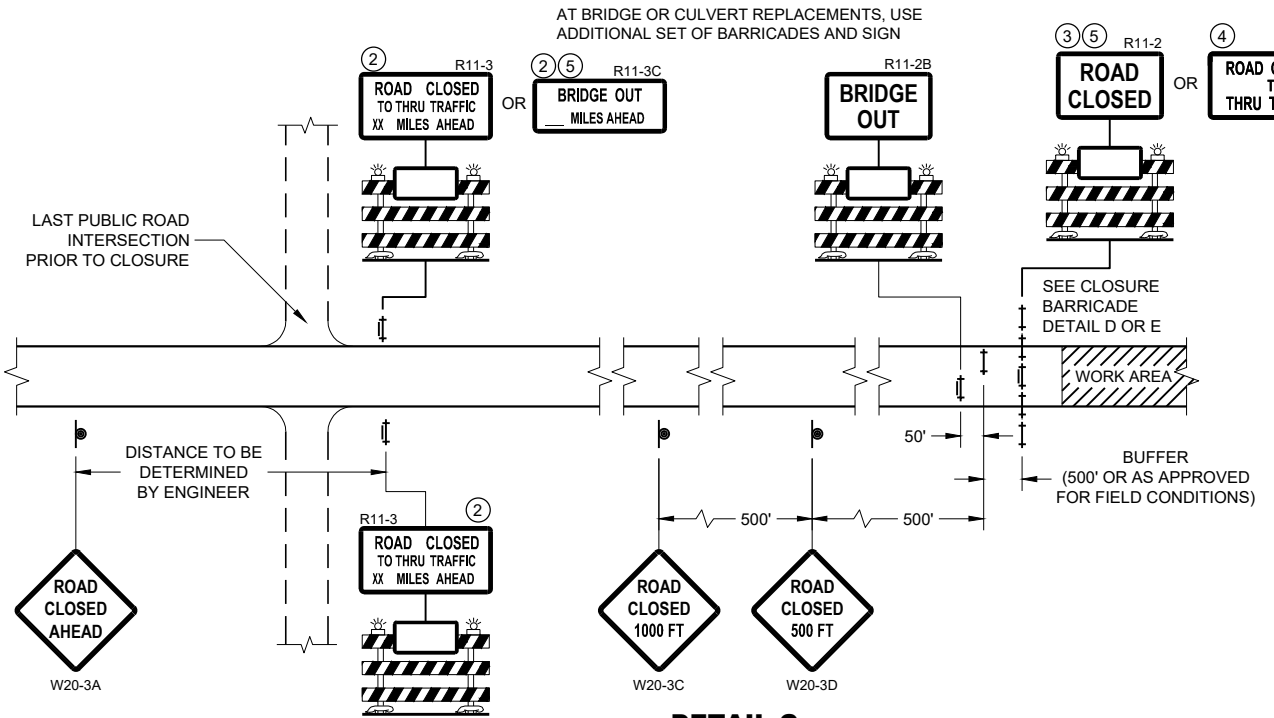
WORK ZONE LESS THAN 1/2 MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
- FLAGS, 16" X 16" MIN. (ORANGE)

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

- M4 - 8
- M3 - X
- M1 - 4
- M1 - 6
- M1 - 5A
- M05 - 1
- M06 - 1



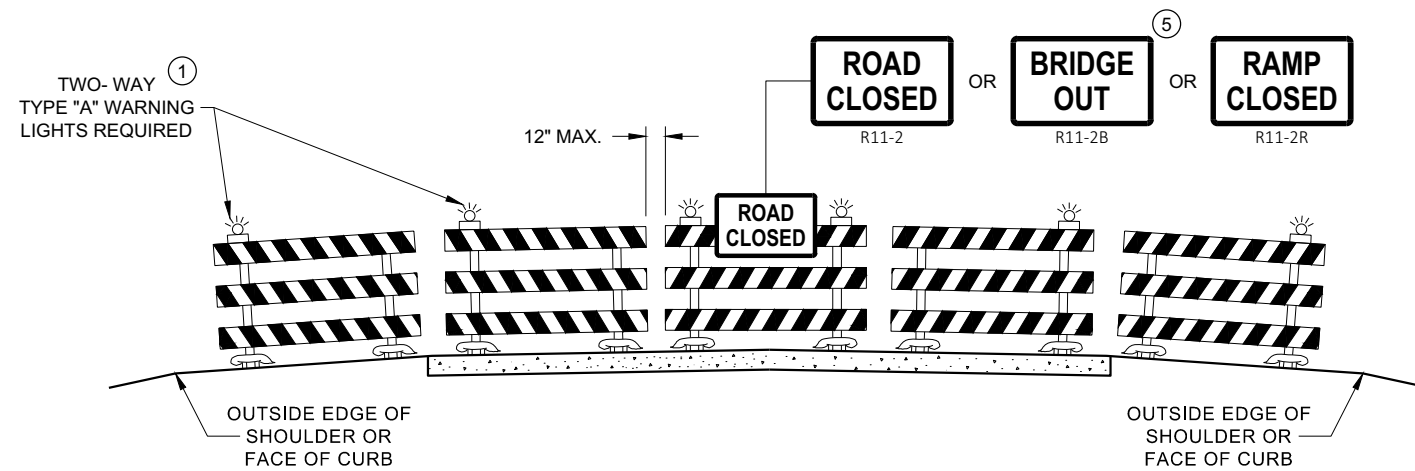
**DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR**

SEE SDD 15C2-SHEET "b"
FOR GENERAL NOTES
AND FOOTNOTES ① THROUGH ⑦

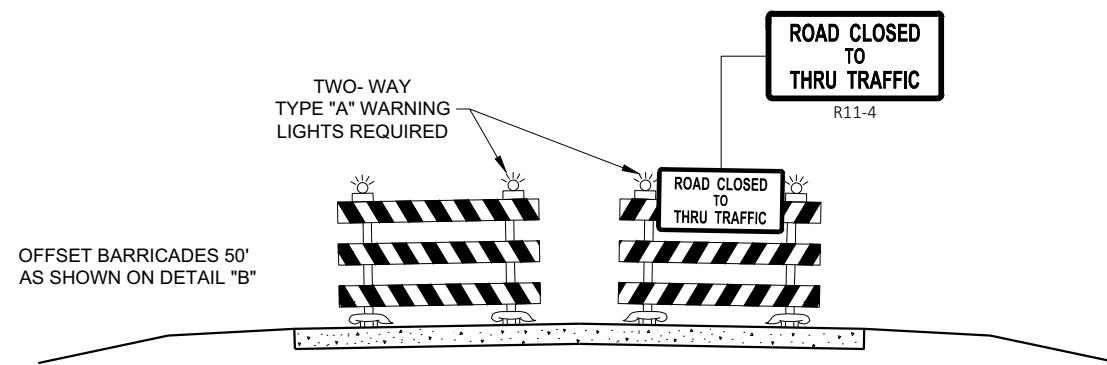
**BARRICADES AND SIGNS
FOR MAINLINE CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020 /S/ Andrew Heidtke
DATE DATE WORK ZONE ENGINEER
FHWA



**DETAIL D
ROAD CLOSURE BARRICADE DETAIL
APPROACH VIEW**



**DETAIL E
LANE CLOSURE BARRICADE DETAIL
APPROACH VIEW**

SEE SDD 15C2 - SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW - INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11 - 2, R11 - 3, M4 - 9, R11 - 4, AND R10 - 61 SIGNS PLACED ON THE BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE RAIL OR BOTTOM RAILS.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

- R11 - 2 SHALL BE 48" X 30"
- R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60" X 30"
- M4 - 9 SHALL BE 30" X 24"
- M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1 - 1 SHALL BE 36" X 36"

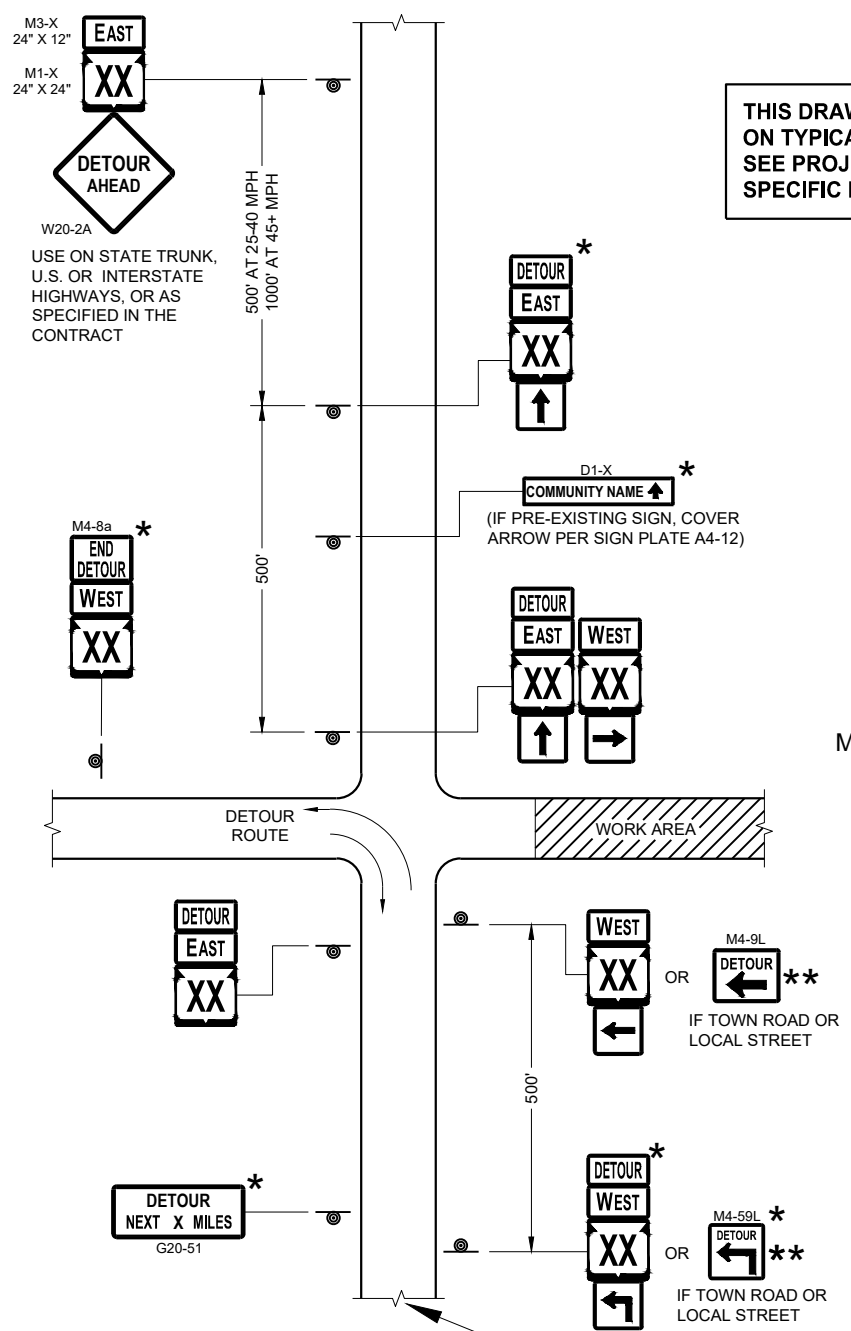
- ① TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT SPACING).
- ② THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.
- ③ FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".
- ④ FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".
- ⑤ FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.
- ⑥ INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.
- ⑦ "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

**BARRICADES AND SIGNS
FOR
VARIOUS CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA



THIS DRAWING PROVIDES GENERAL GUIDANCE ON TYPICAL DETOUR SIGN LAYOUT AND SPACING. SEE PROJECT DETOUR SIGNING SHEETS FOR SPECIFIC DETAILS FOR EACH PROJECT.

LEGEND

- SIGN ON PERMANENT SUPPORT
- WORK AREA
- M4 - 8
- M3 - X
- M1 - 4
- M1 - 6
- M1 - 5A
- M05 - 1
- M06 - 1
- M06 - 1

GENERAL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. MODIFY EXISTING SIGNS WHERE POSSIBLE.

THE SPACING BETWEEN TRAFFIC CONTROL AND DETOUR SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

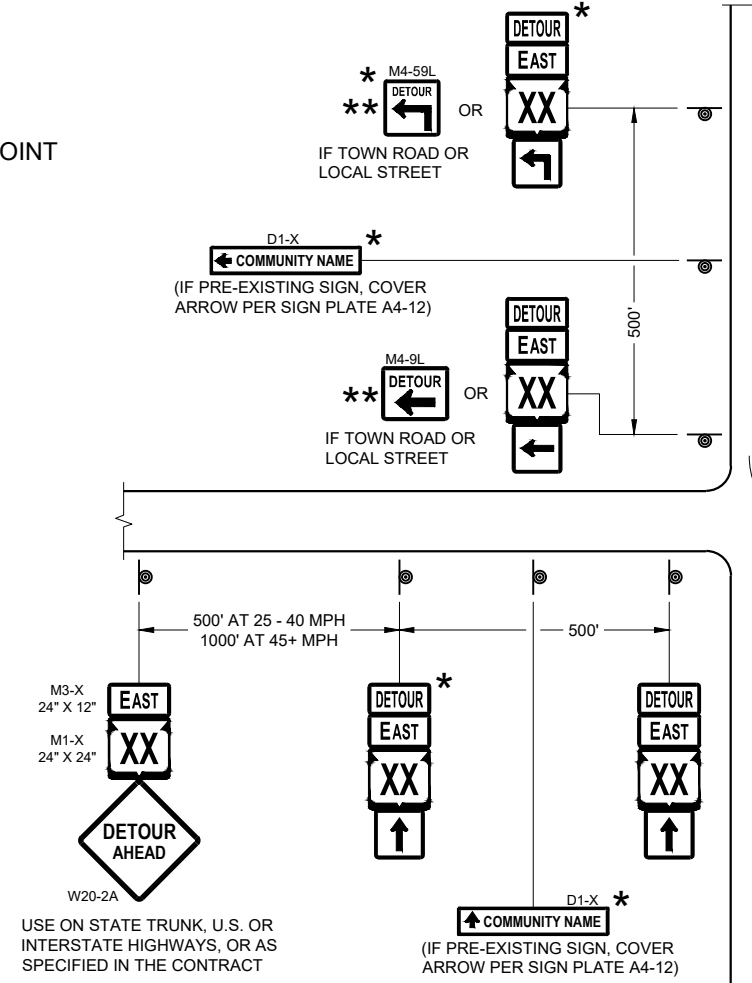
"MO" SIGNS ARE THE SAME AS "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGN SIZES SHALL BE AS FOLLOWS:

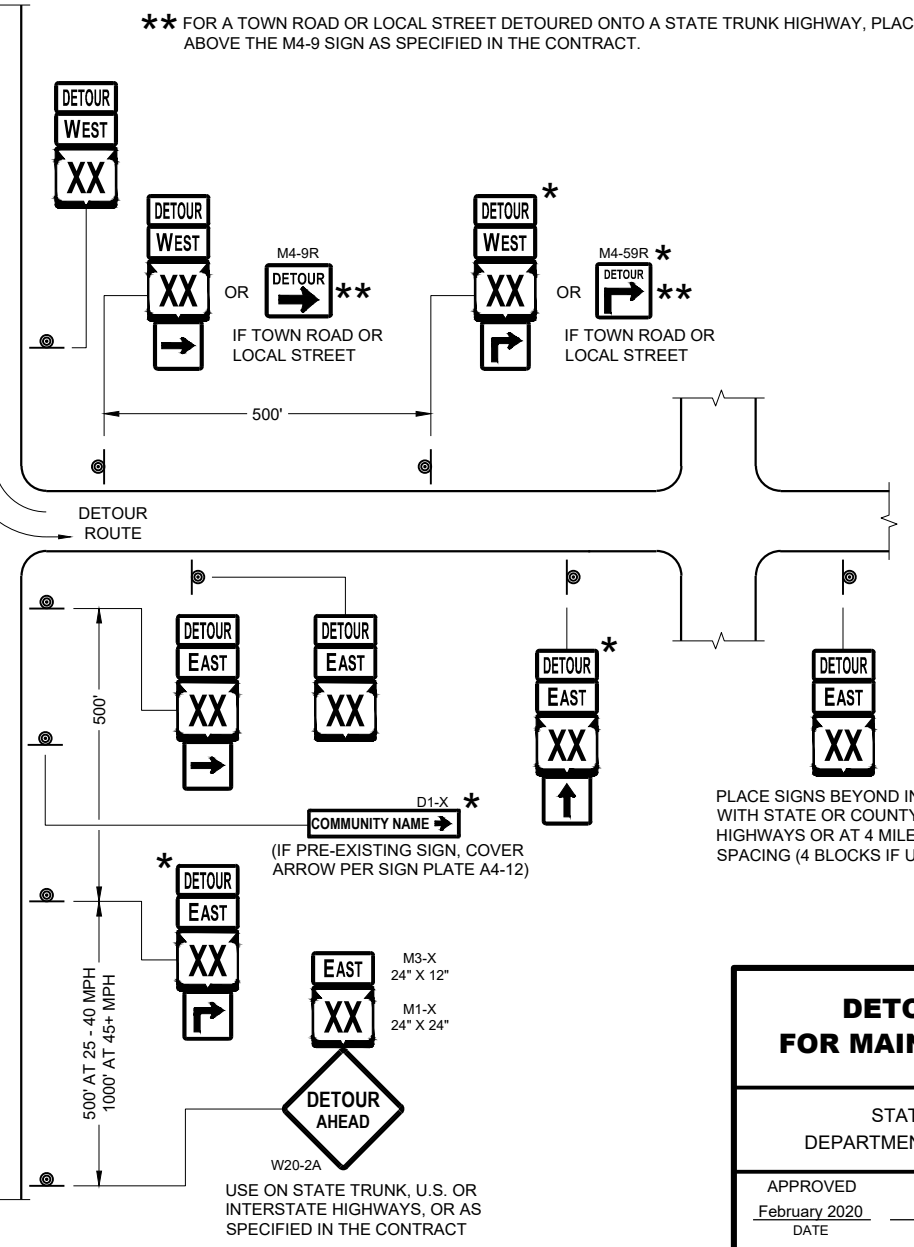
- M3-X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4-8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1-4, M1-5A AND M1-6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- M05-1 AND M06-1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- M4-9 AND M4-59 SHALL BE 30" X 24"
- M4-8a SHALL BE 24" X 18"
- G20-51 SHALL BE 60" X 24"
- W20-2A SHALL BE 48" X 48"
- D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.

- * OPTIONAL SIGNS. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS.
- ** FOR A TOWN ROAD OR LOCAL STREET DETOURED ONTO A STATE TRUNK HIGHWAY, PLACE A ROAD NAME PLAQUE ABOVE THE M4-9 SIGN AS SPECIFIED IN THE CONTRACT.

MATCH POINT



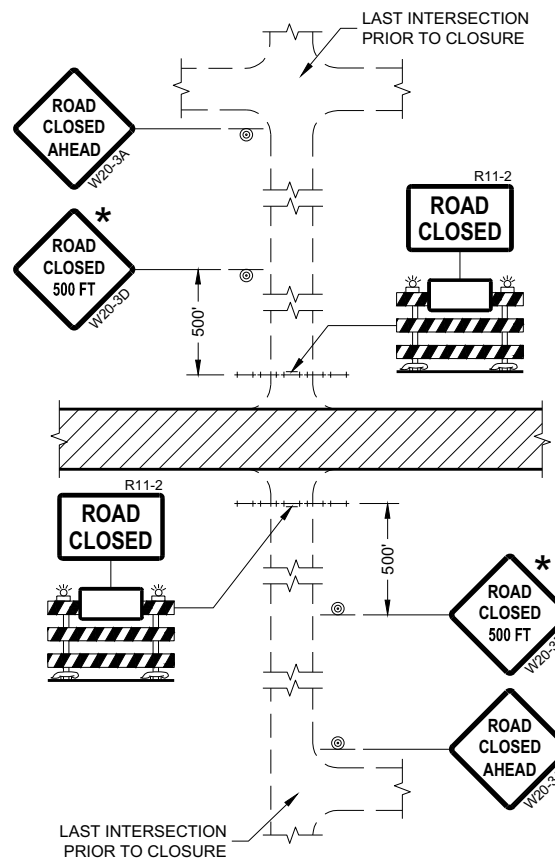
**DETAIL F
DETOUR SIGNING**



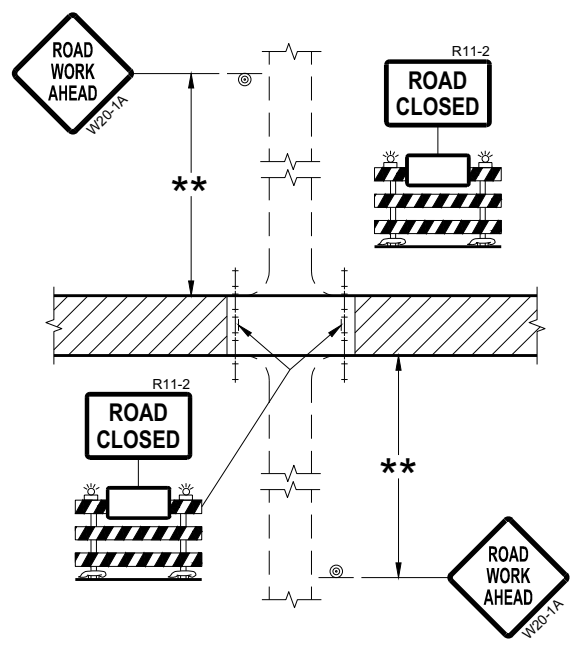
PLACE SIGNS BEYOND INTERSECTIONS WITH STATE OR COUNTY TRUNK HIGHWAYS OR AT 4 MILE MAXIMUM SPACING (4 BLOCKS IF URBAN AREA)

SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS AND DETAIL A OR B ON SDD SHEET 15C02 - SHEET "a"

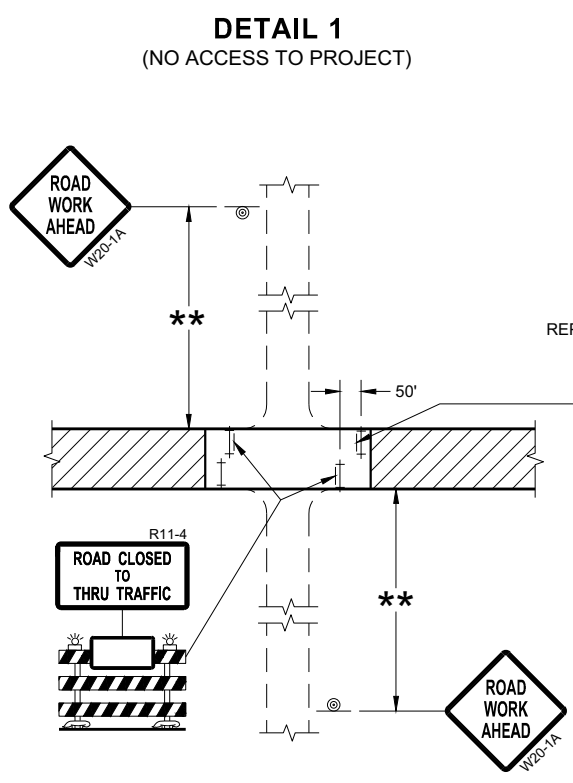
DETOUR SIGNING FOR MAINLINE CLOSURES	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED February 2020 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	



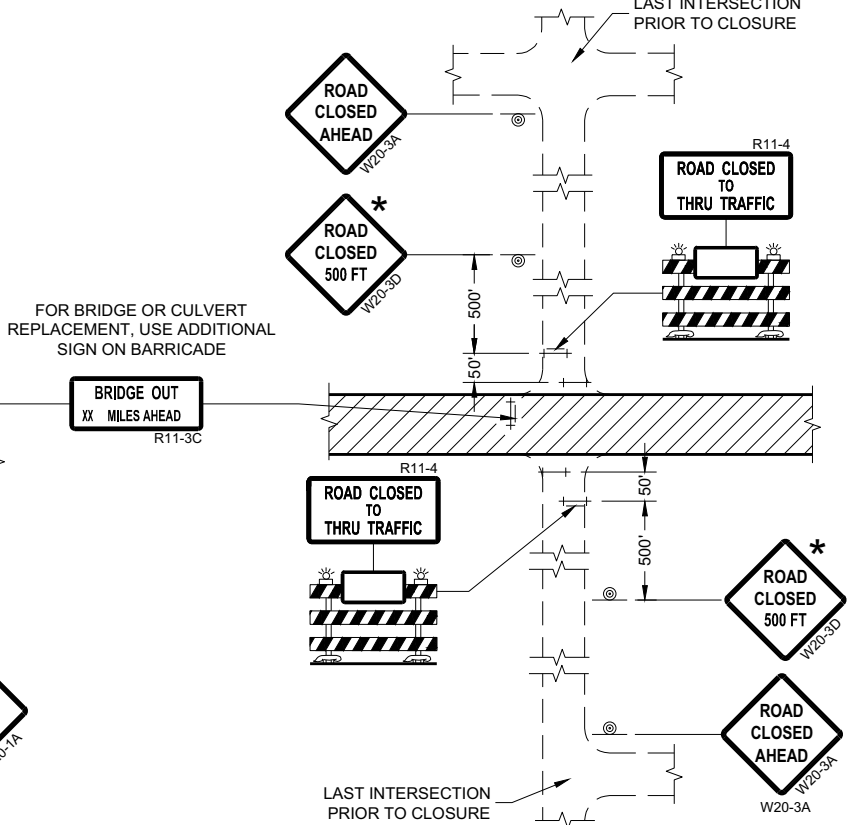
DETAIL 1
(NO ACCESS TO PROJECT)



DETAIL 2
(PUBLIC CROSS-TRAFFIC MAINTAINED.
NO ACCESS TO PROJECT)



DETAIL 3
(PUBLIC CROSS-TRAFFIC MAINTAINED.
CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)



DETAIL 4
(CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE (500 FEET DESIRABLE) TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3, AND R11-4 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
R11-2 SHALL BE 48" X 30".
R11-4 AND R11-3 SHALL BE 60" X 30".

- * OMIT THE "ROAD CLOSED 500 FT." SIGN IF THE LAST INTERSECTION IS 500 FEET OR LESS FROM THE WORK ZONE.
- ** 500' MAX. OR AT LAST INTERSECTION, WHICHEVER IS CLOSEST.

LEGEND

- ⊙ SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- ⚡ TYPE "A" WARNING LIGHT (FLASHING)
- ▨ WORK AREA

**BARRICADES AND SIGNS
FOR
SIDEROAD CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION




APPROVED
July 2018 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER
FHWA

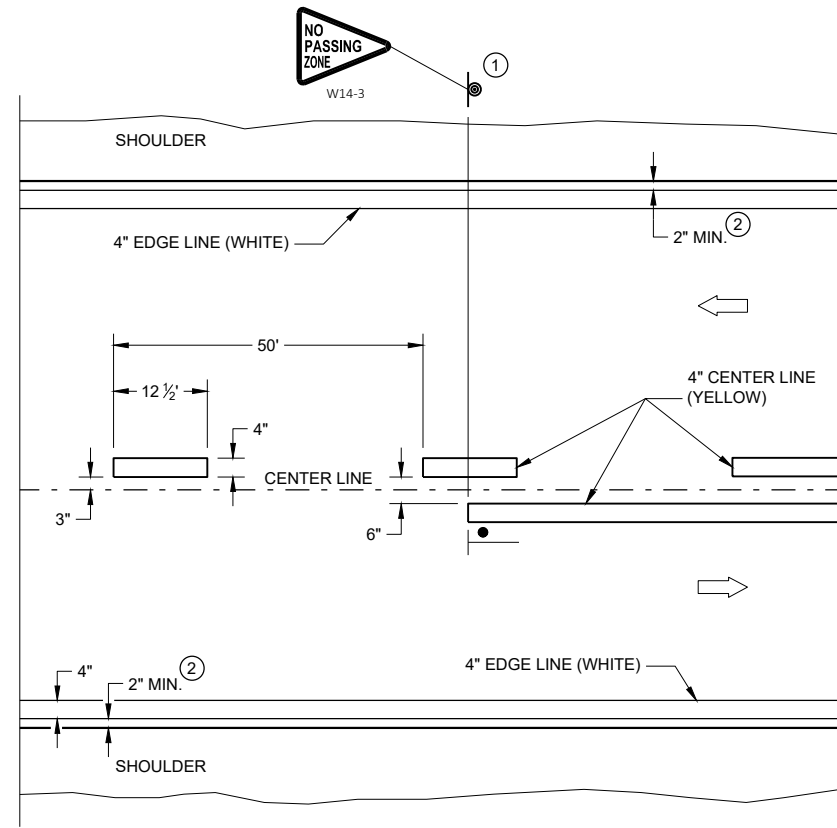
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

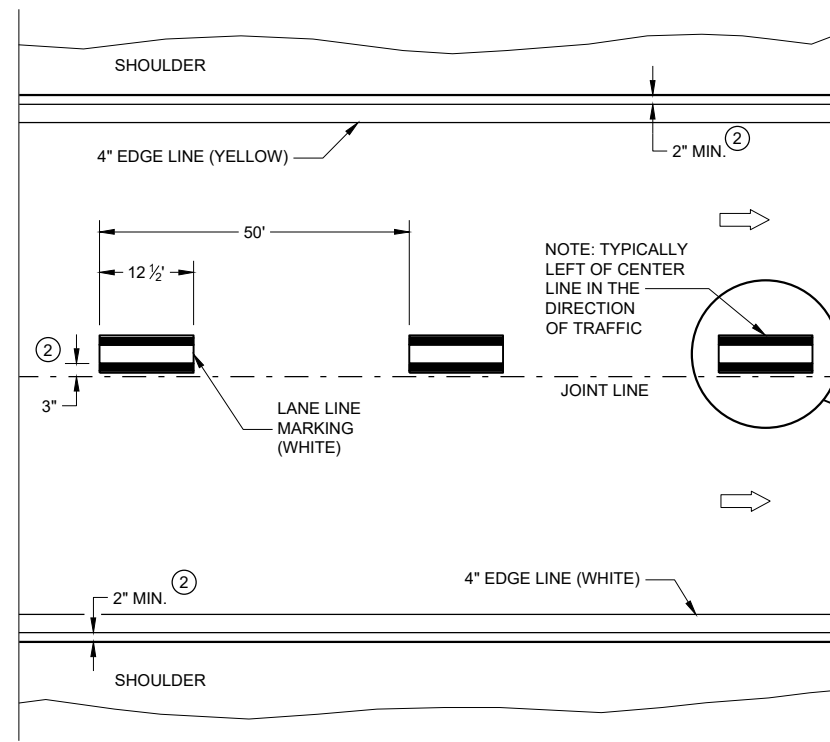
- ① LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING
- ② MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

LEGEND

-  "T" MARKING
-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC

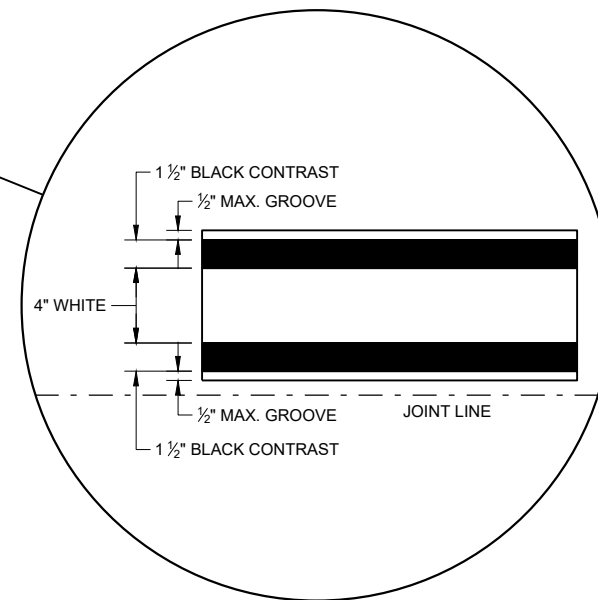


TWO WAY TRAFFIC



ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING



6

6

SDD 15C08 - 21a

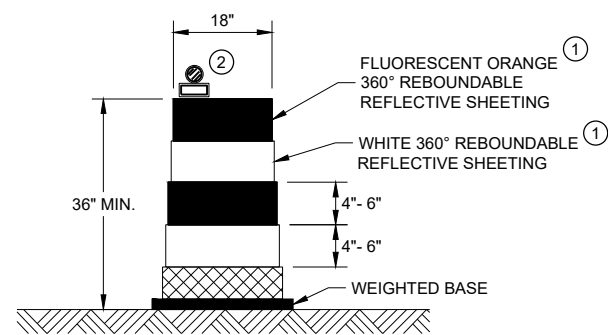
SDD 15C08 - 21a

PERMANENT LONGITUDINAL PAVEMENT MARKINGS

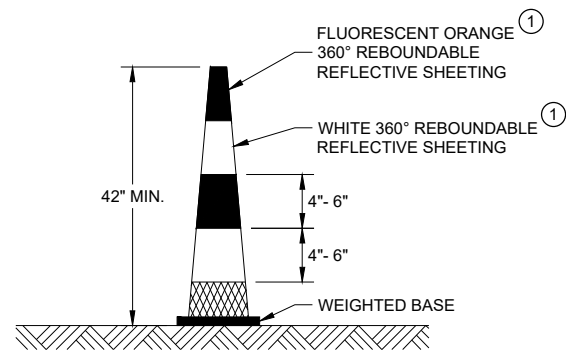
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE: May 2022 /S/ Jeannie Silver
STATEWIDE SIGNING AND MARKING ENGINEER

FHWA

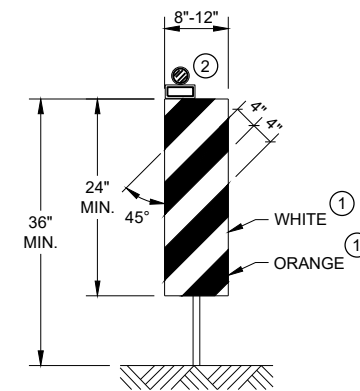


DRUM



42" CONE

DO NOT USE IN TAPERS
½ SPACING OF DRUMS

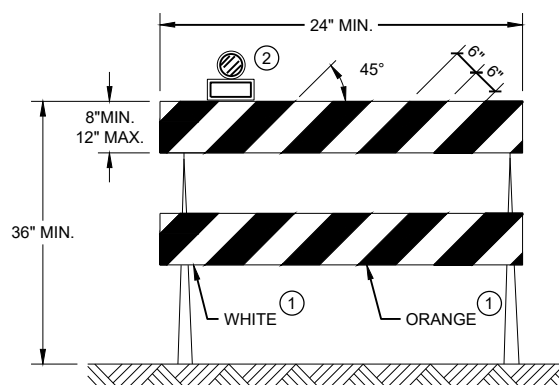


VERTICAL PANEL

THE STRIPES SHALL SLOPE DOWNWARD TO
THE TRAFFIC SIDE FOR CHANNELIZATION.

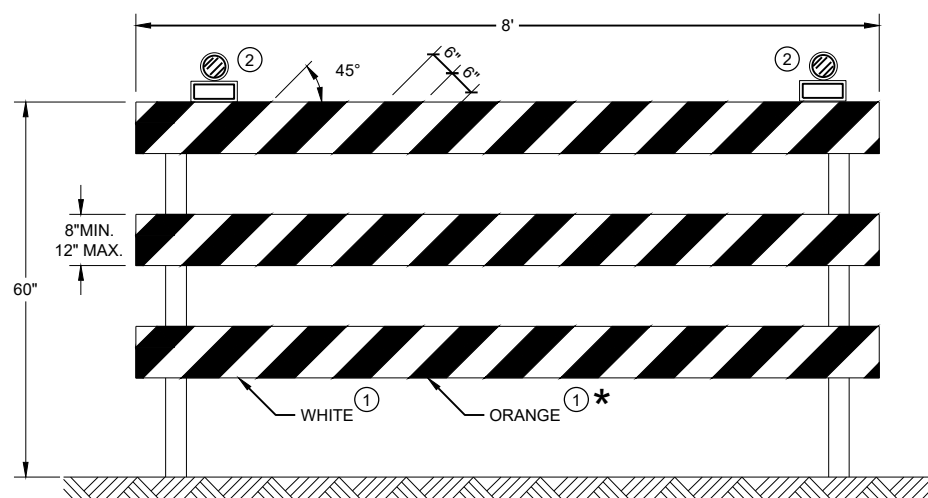
GENERAL NOTES

- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.



TYPE II BARRICADE

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES
MAY BE USED. ALL STRIPES SHALL SLOPE DOWNWARD
TO THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE III BARRICADE

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP
TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.

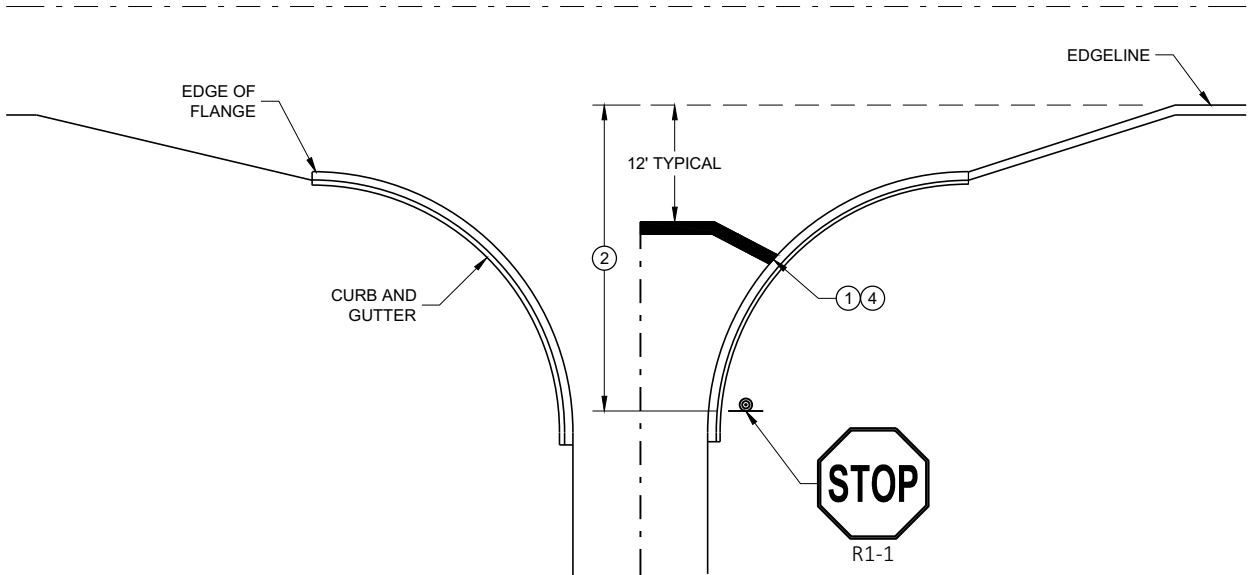
* IF USED FOR A PERMANENT APPLICATION USE RED SHEETING.

CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2021 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
<small>FHWA</small>	

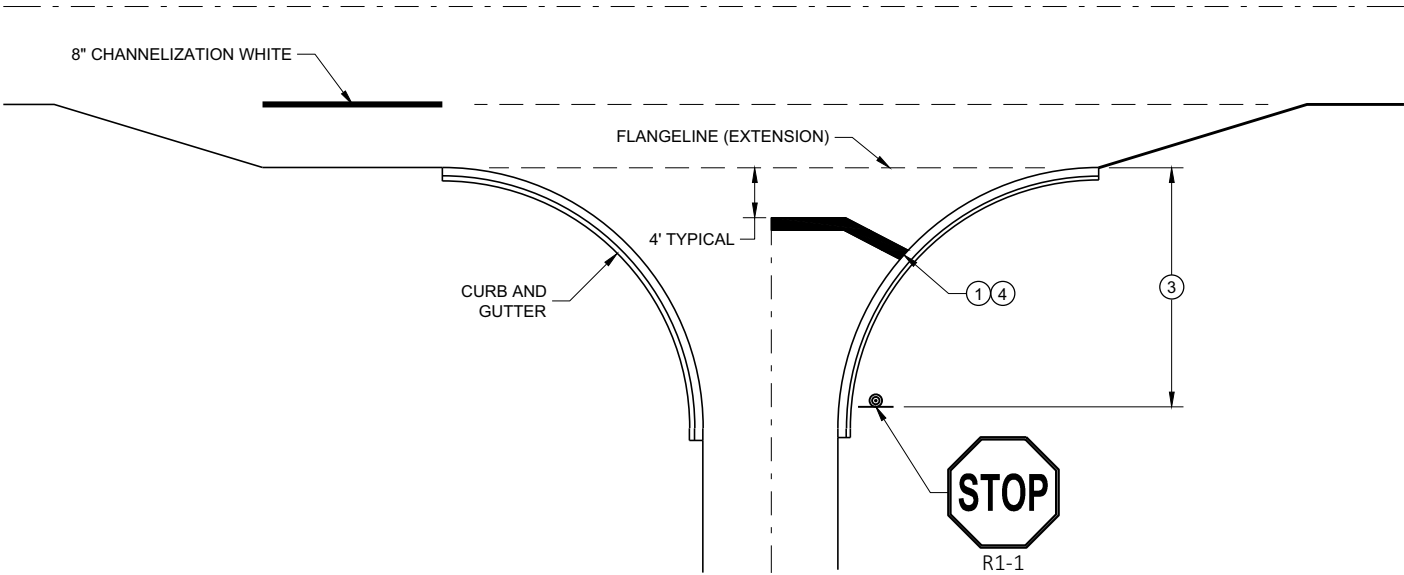
GENERAL NOTES

STOP SIGN SHALL BE PLACED A MINIMUM OF 6 FEET TO A MAXIMUM OF 50 FEET FROM THE EDGELINE LOCATION.

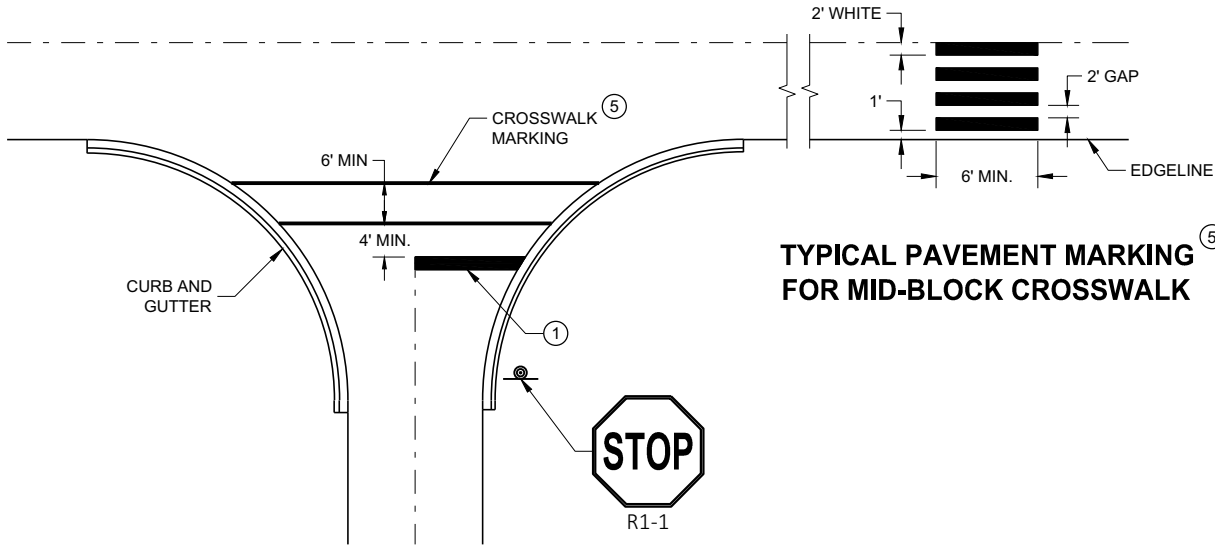
- ① 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE REGION MARKING ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
- ② NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGELINE.
- ③ NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGELINE EXTENSION.
- ④ MOVE CLOSER TO THE EDGE OF TRAVEL LINE AS NEEDED FOR VISIBILITY AND SIGHT LINES (NO CLOSER THAN 4 FEET).
- ⑤ LADDER BAR CROSSWALKS SHOULD ONLY BE USED FOR MID BLOCK CROSSINGS. USE 2 - 6" TRANSVERSE LINES INSTEAD.



TYPICAL STOP LINE PAVEMENT MARKING WITH CURB AND GUTTER

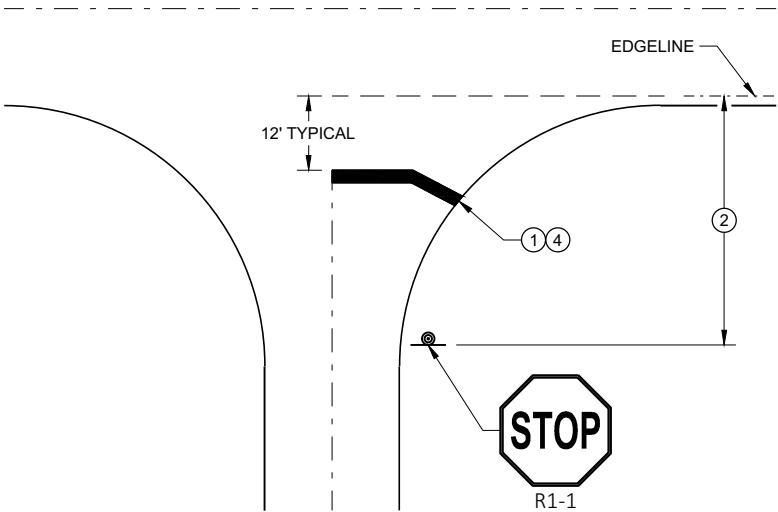


TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH RIGHT TURN LANE



TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH CROSSWALK MARKING

TYPICAL PAVEMENT MARKING FOR MID-BLOCK CROSSWALK



TYPICAL STOP LINE PAVEMENT MARKING WITHOUT CURB AND GUTTER

STOP LINE AND CROSSWALK PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2019 /S/ Matthew Rauch
DATE STATE SIGNING AND MARKING ENGINEER

FHWA

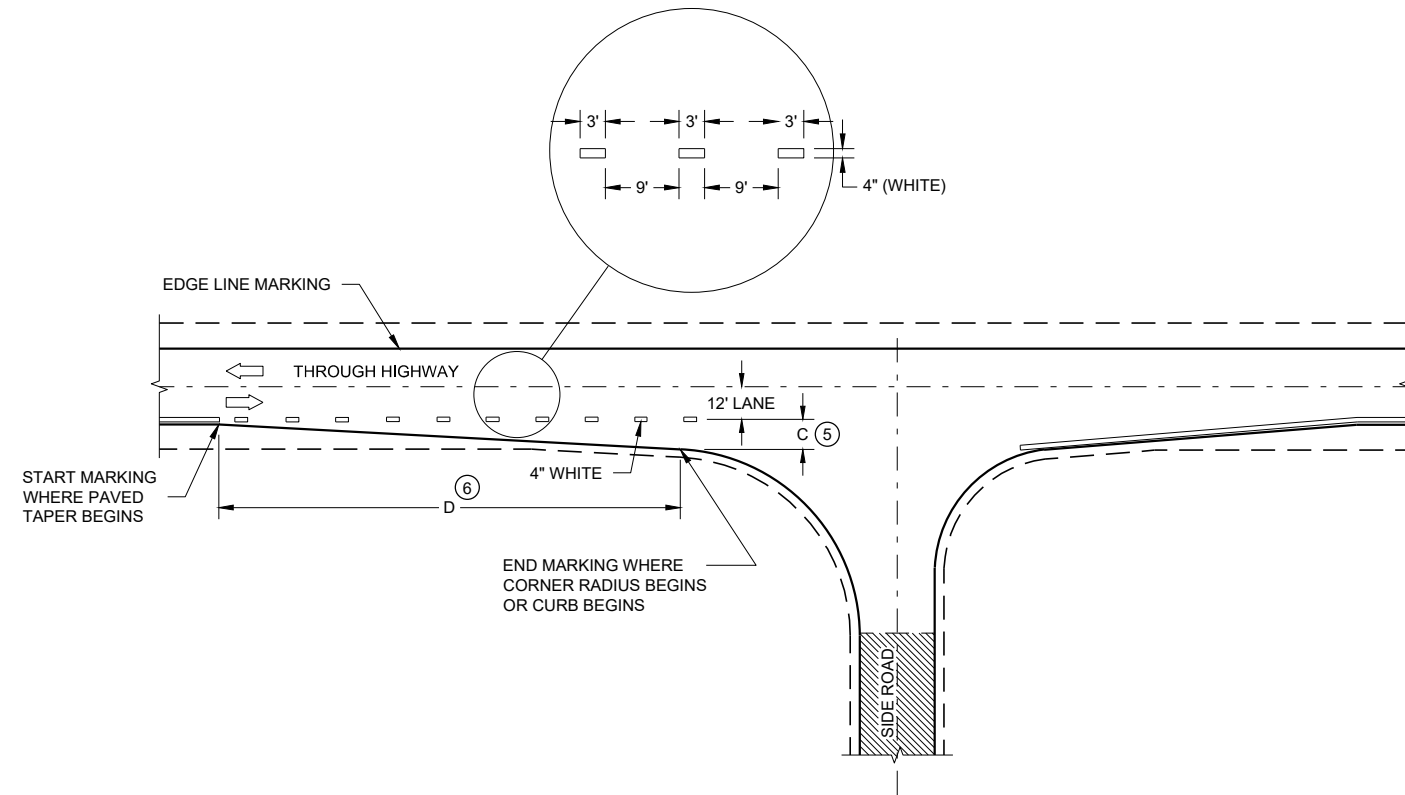
GENERAL NOTES

OMIT EDGE LINES THROUGH INTERSECTIONS. CONTINUE EDGE LINES THROUGH DRIVEWAYS.

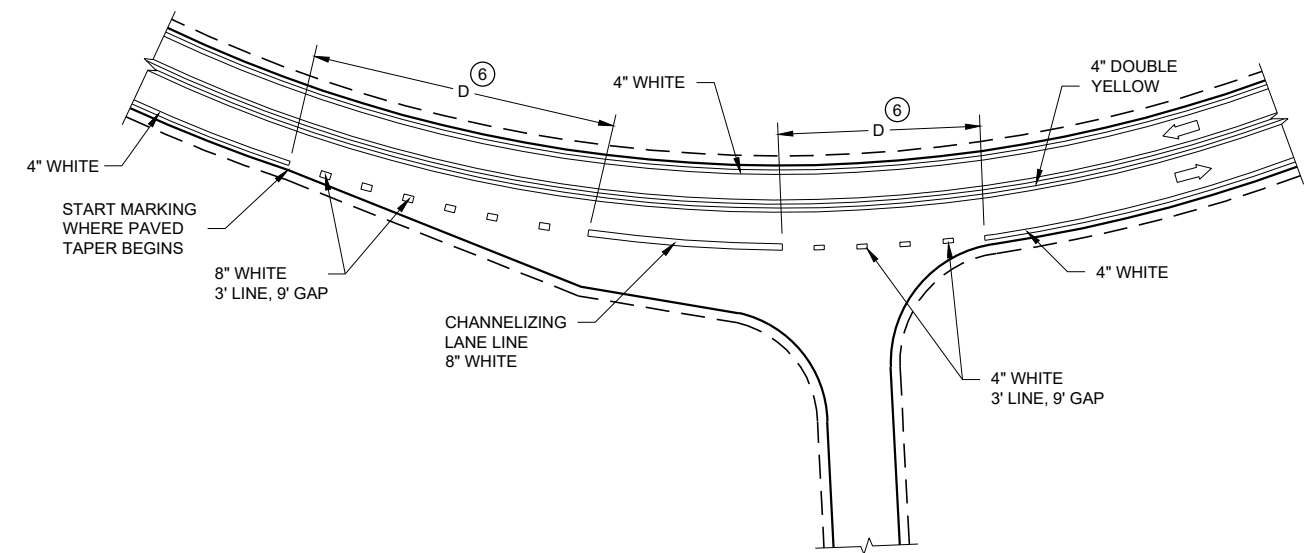
- ① WHEN DISTANCE "A" IS LESS THAN 250 FEET, OMIT LANE LINE.
- ② WHEN DISTANCE "B" IS LESS THAN 100 FEET, OMIT CHANNELIZING LANE LINE.
- ③ BARRIER LINE ENDS AT SIDE ROAD PAVEMENT / SURFACE EDGE EXTENSION.
- ④ BARRIER LINE STARTS 500 FEET PRIOR TO THE BYPASS TAPER.
- ⑤ WHEN DISTANCE "C" IS LESS THAN 4 FEET, OMIT DOTTED EXTENSION.
- ⑥ WHEN DISTANCE "D" IS LESS THAN 50 FEET, OMIT DOTTED EXTENSION.

LEGEND

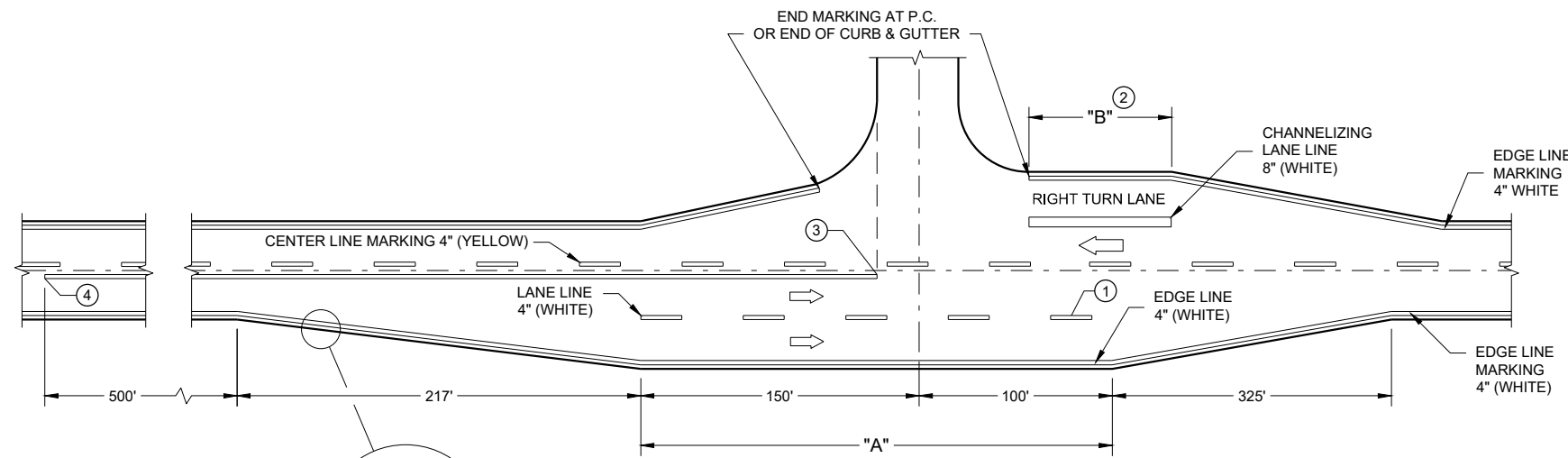
➡ DIRECTION OF TRAVEL



MINOR INTERSECTION



INTERSECTION ON OUTSIDE OF CURVE



**MAJOR INTERSECTIONS
(INTERSECTION WITH FULL RIGHT TURN LANE OR BYPASS LANE)**

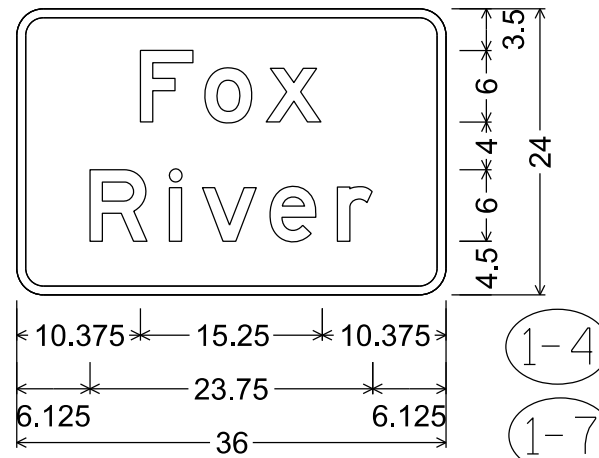
BYPASS LANE PAVED SHOULDER WIDTH (AS SHOWN ELSEWHERE IN PLANS) - PLUS 2 INCHES

**PAVEMENT MARKING
(INTERSECTIONS)**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

NOTES

1. Sign is Type II- Type H Reflective
2. Color:
Background - Green
Message - White
3. Message Series - E

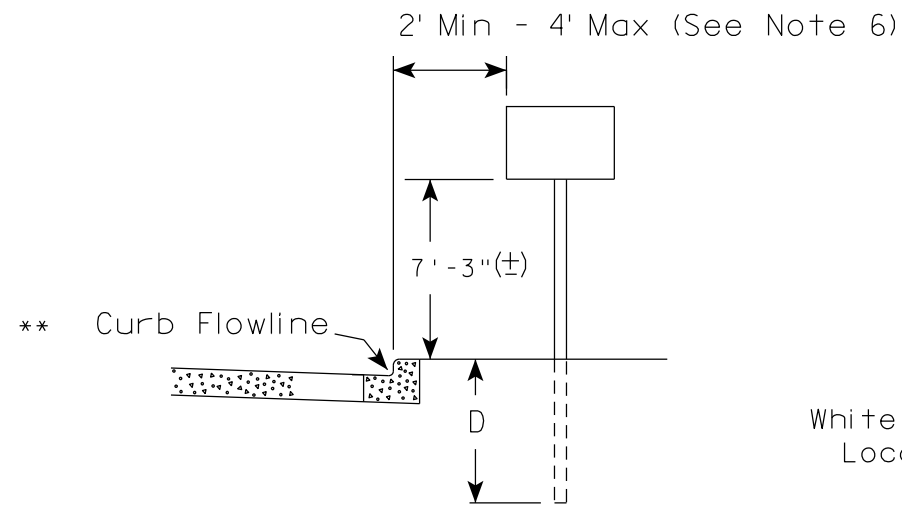


I3-1; 2.250" Radius, 0.750" Border

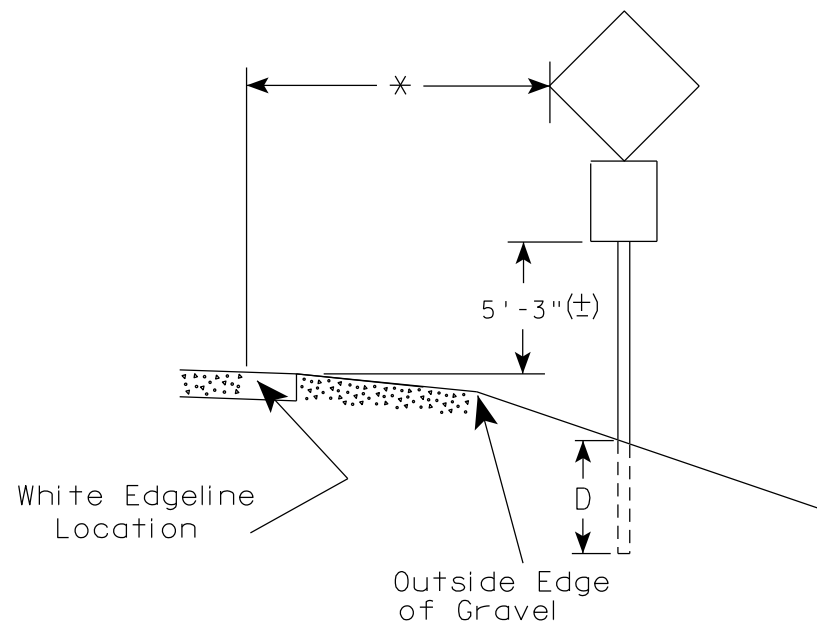
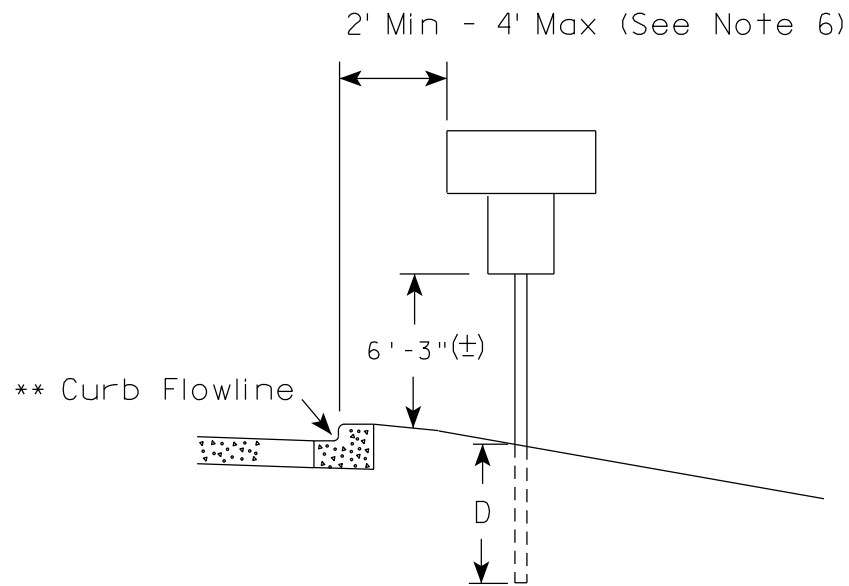
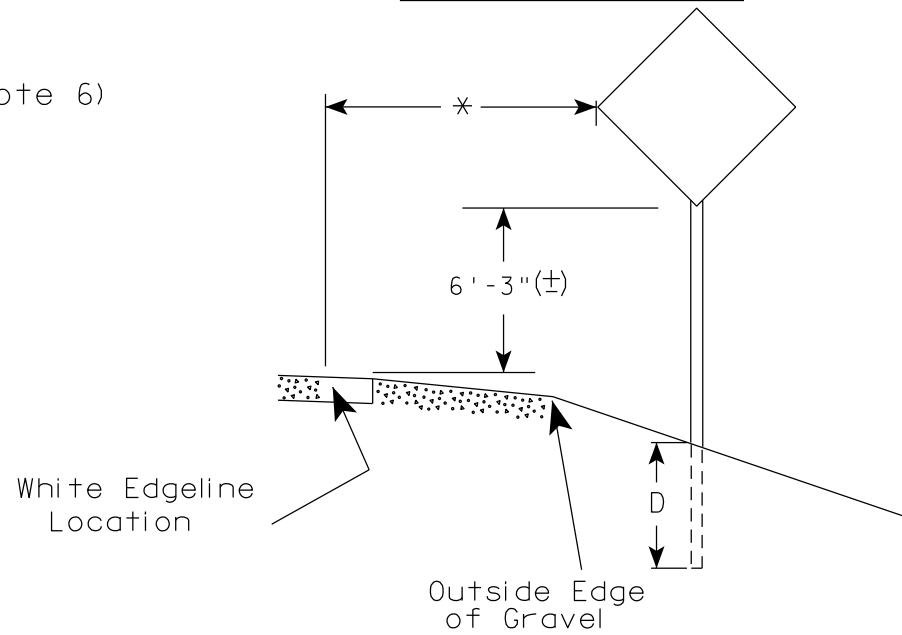
7

7

URBAN AREA



RURAL AREA (See Note 2)



POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

GENERAL NOTES

1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on or behind barrier wall, see A4-10 sign plate.
The Double Arrow sign (W12-1D) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).
3. For expressways and freeways, mounting height is 7'- 3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
4. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±).
5. Offset distance shall be consistent with existing signs or consistent throughout length of project.
6. The (±) tolerance for mounting height is 3 inches.
7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the Engineer.

* * The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

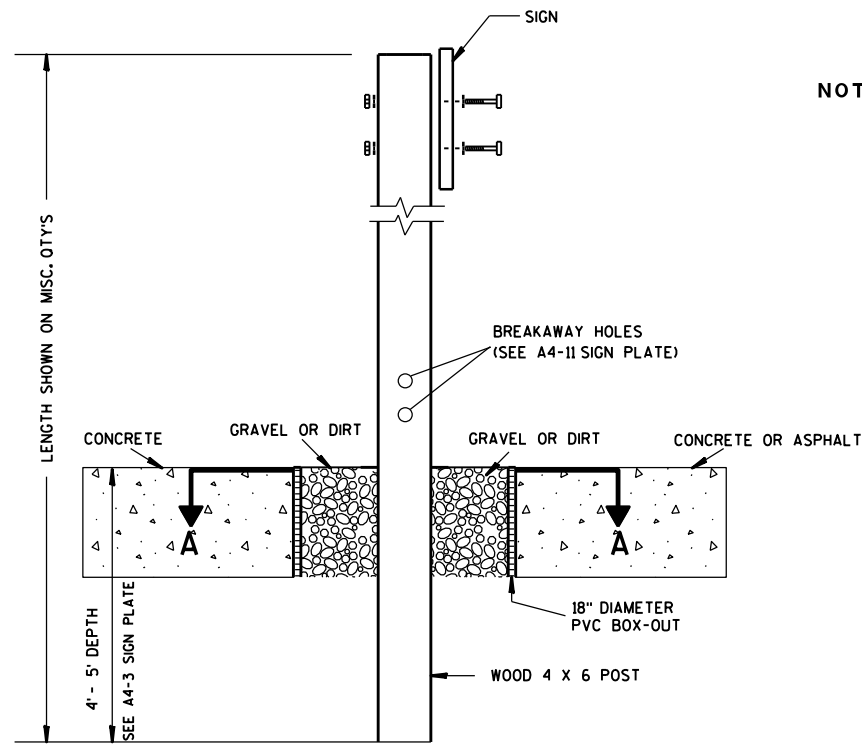
* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

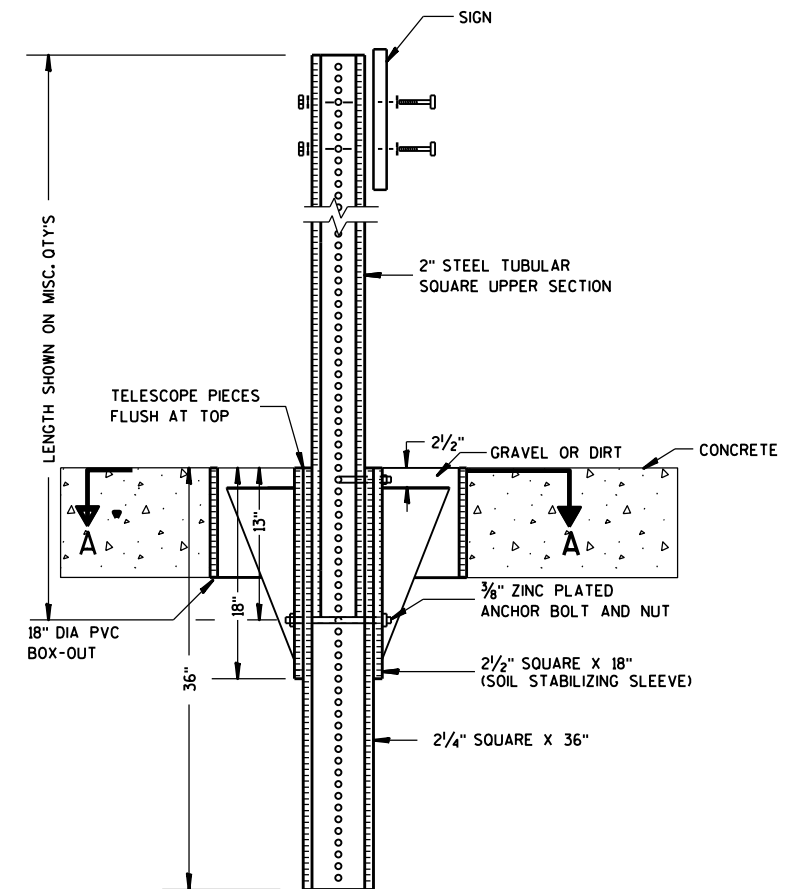
DATE 5/13/2020 PLATE NO. A4-3.22



ELEVATION VIEW

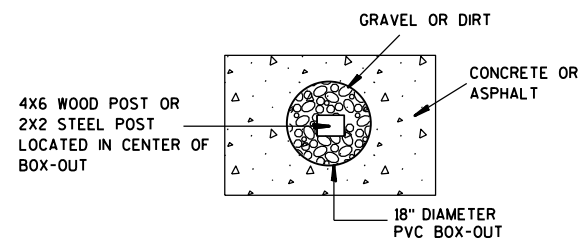
DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

- NOTES:**
1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
 2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
 3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



PLAN VIEW

FOR NEW CONCRETE/ ASPHALT INSTALLATIONS

**SIGN POST
BOX-OUTS
A4-3B**

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 1/27/14 PLATE NO. A4-3B.1

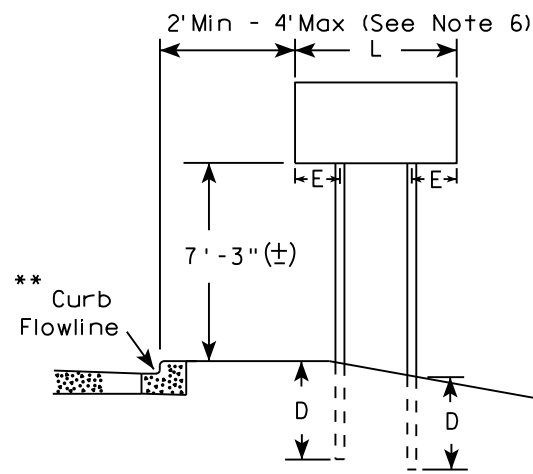
7

7

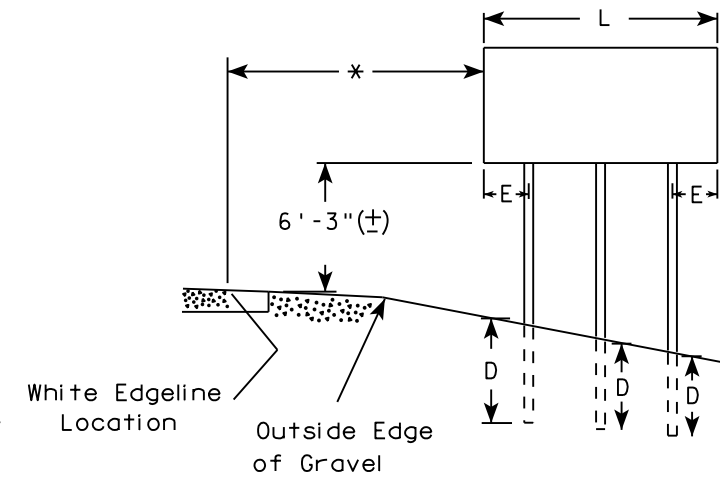
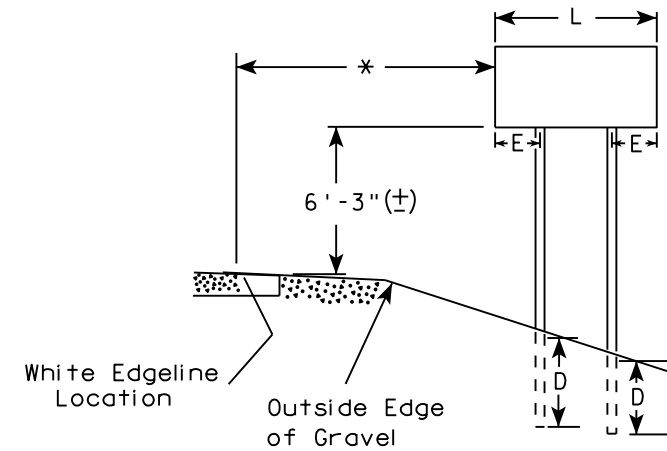
GENERAL NOTES

1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
2. See tables below for required number of posts.
3. For expressways and freeways, mounting height is 7'-3" (±) or 6'-3" (±) depending upon existence of sub-sign.
4. The (±) tolerance for mounting height is 3 inches.
5. J-Assemblies are considered to be one sign for mounting height.
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the engineer.
8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).

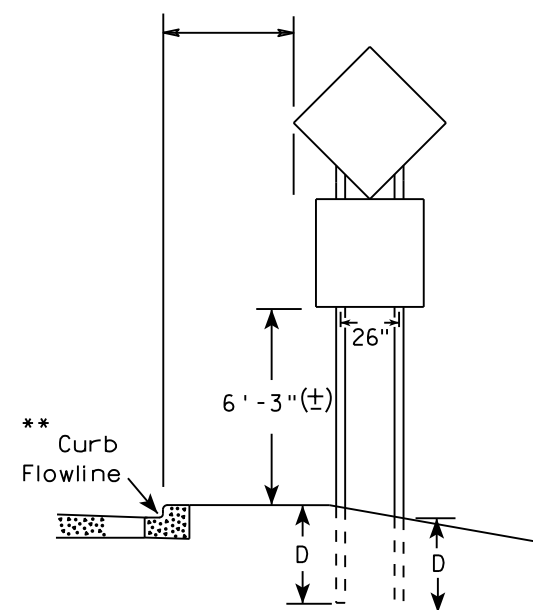
URBAN AREA



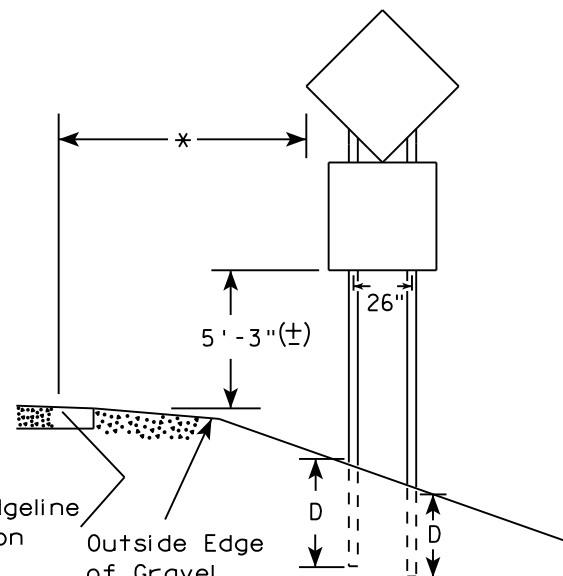
RURAL AREA (See Note 3)



2' Min - 4' Max (See Note 6)



48" DIAMOND WARNING SIGN



48" DIAMOND WARNING SIGN

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

*** See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)	
L	E
Greater than 48" Less than 60"	12"
60" to 108"	L/5

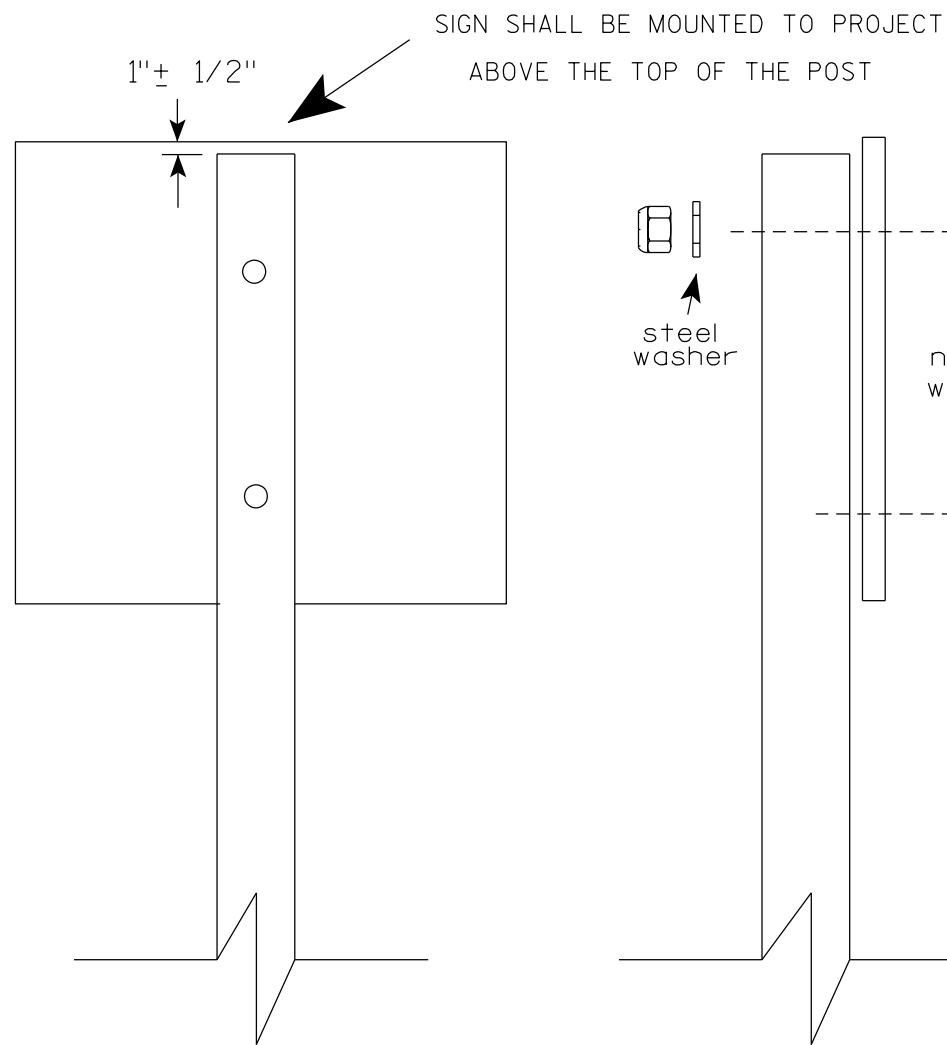
SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)	
L	E
Greater than 108" to 144"	12"

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS

WISCONSIN DEPT OF TRANSPORTATION
 APPROVED *Matthew R. Rauch*
 For State Traffic Engineer
 DATE 8/21/17 PLATE NO. A4-4.15



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

- Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
- Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

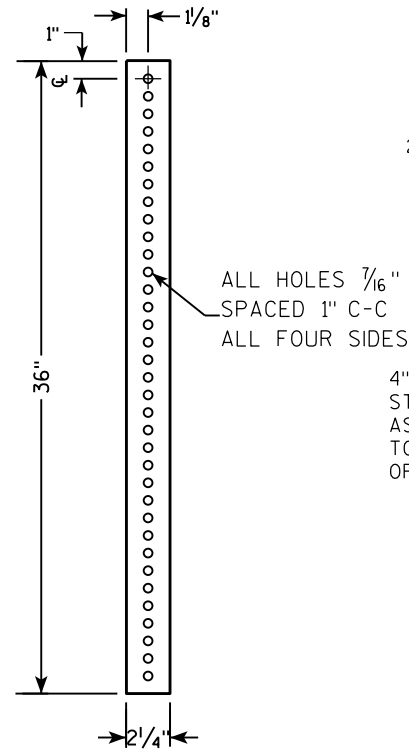
- STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)
- MACHINE BOLTS - $\frac{5}{16}$ " X 1-3/4" Length w/ lock nuts
- WOOD POSTS (4" x 6")
- LAG SCREWS - $\frac{3}{8}$ " X 3" (NO STRINGERS ON BACK OF SIGN)
 - $\frac{3}{8}$ " X 4" (STRINGERS ON BACK OF SIGN)
- SQUARE STEEL POSTS (2" x 2")
- MACHINE BOLTS - $\frac{3}{8}$ " X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)
 - $\frac{3}{8}$ " X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)
- RIVETS - $\frac{9}{32}$ " (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
- O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH
- WASHERS (ALL POSTS) -
- 1-1/4" O.D. X $\frac{3}{8}$ " I.D. X $\frac{1}{16}$ " STEEL
 - 1-1/4" O.D. X $\frac{3}{8}$ " I.D. X .080 NYLON

* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

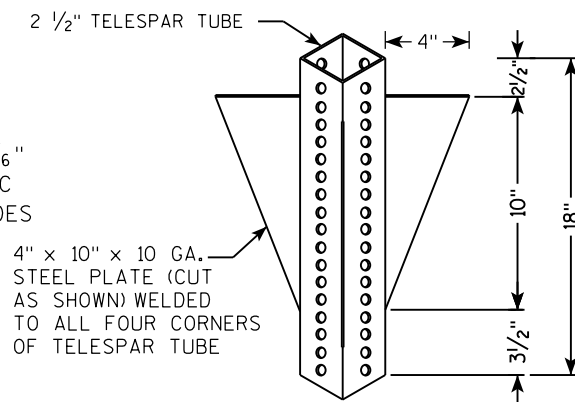
ATTACHMENT OF SIGNS TO POSTS	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R Rauch</i> For State Traffic Engineer
DATE 4/1/2020	PLATE NO. A4-8.9

**TELESCOPIC TUBING ANCHORS
TWO PIECE SYSTEM**

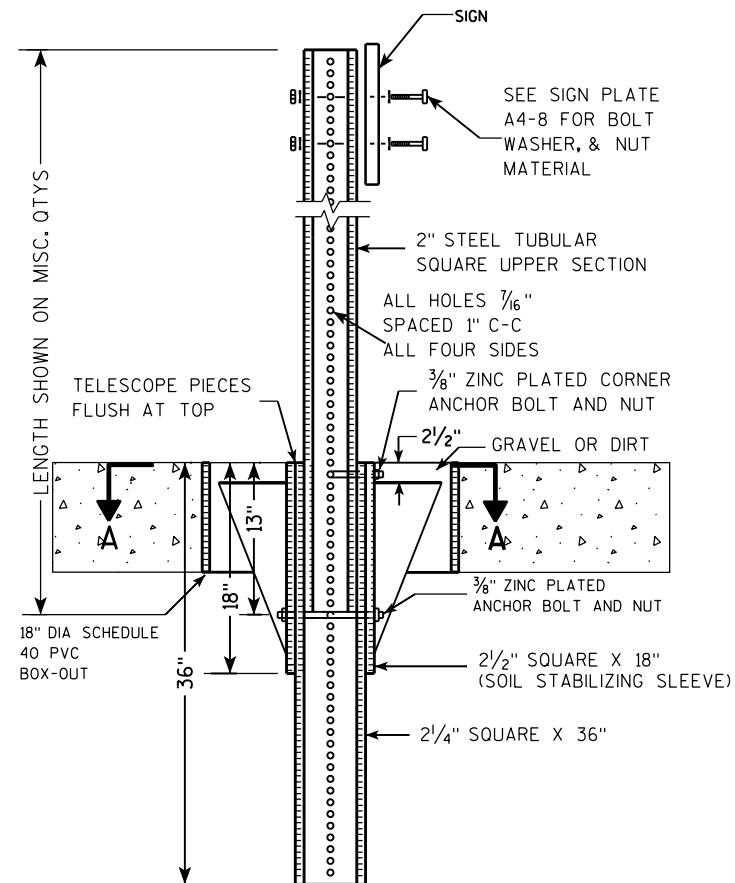
2 1/4" SQUARE
12 GAUGE
PERFORATED
GALVANIZED FINISH



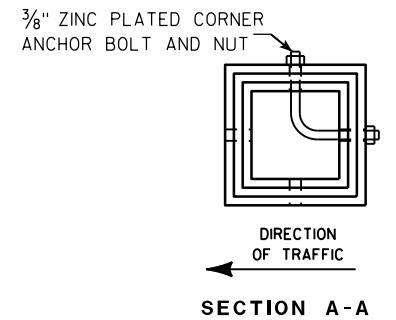
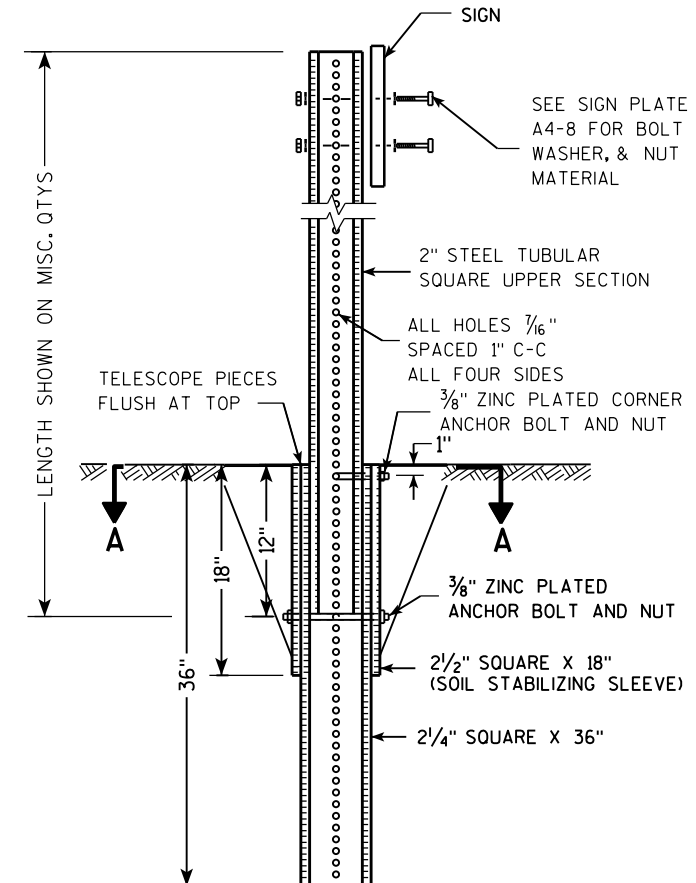
2 1/2" SQUARE
12 GAUGE
OMNI-DIRECTIONAL
PERFORATED
SOIL STABILIZING SLEEVE
GALVANIZED FINISH



**DETAIL OF TUBULAR STEEL SIGN POST
(IN POURED CONCRETE OR ASPHALT)**



**DETAIL OF TUBULAR STEEL SIGN POST
(IN LOCATIONS OTHER THAN POURED CONCRETE OR ASPHALT)**



Area of Sign Installation (Sq. Ft.)	Number of Required Posts
9 or less	1
Greater than 9 less than or equal to 18	2
Greater than 18 less than or equal to 27	3

Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

**TUBULAR STEEL
SIGN POST
A4-9**

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 2/05/15 PLATE NO. A4-9.9

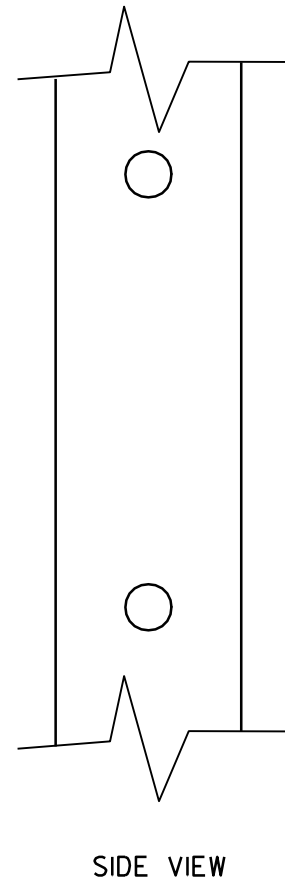
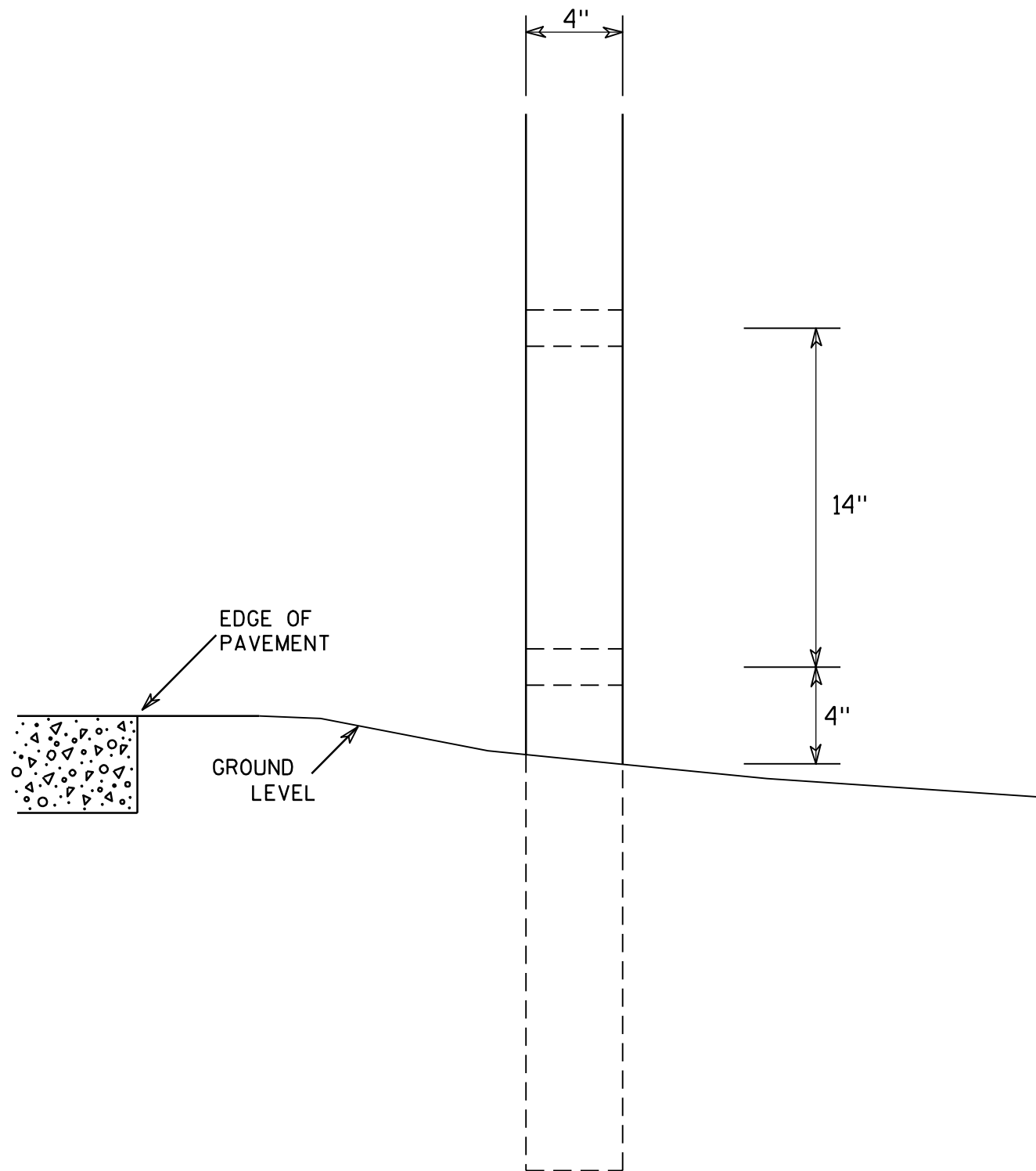
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E



GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

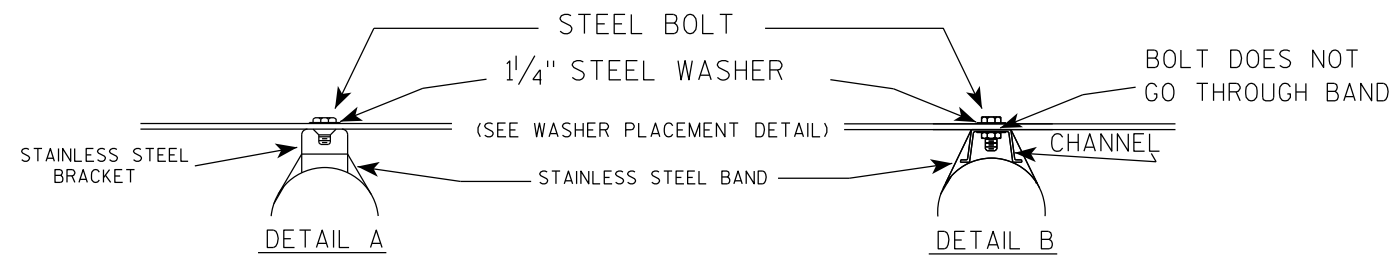
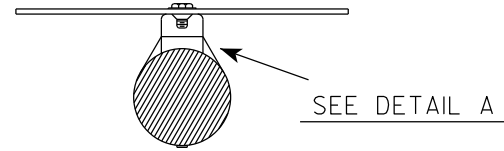
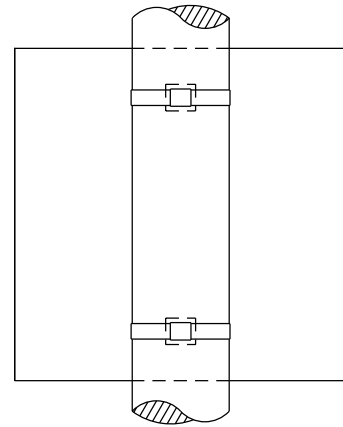
7

7

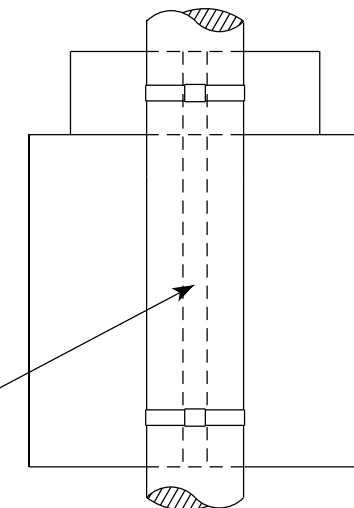
4 X 6 WOOD POST MODIFICATIONS	
<i>WISCONSIN DEPT OF TRANSPORTATION</i>	
APPROVED	<i>Chester J Spang</i> for State Traffic Engineer
DATE <u>3/27/97</u>	PLATE NO. <u>A4-11.2</u>

BANDING

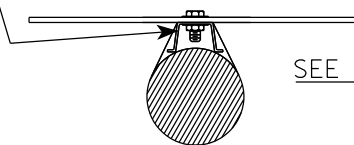
SINGLE SIGN



"J" ASSEMBLY

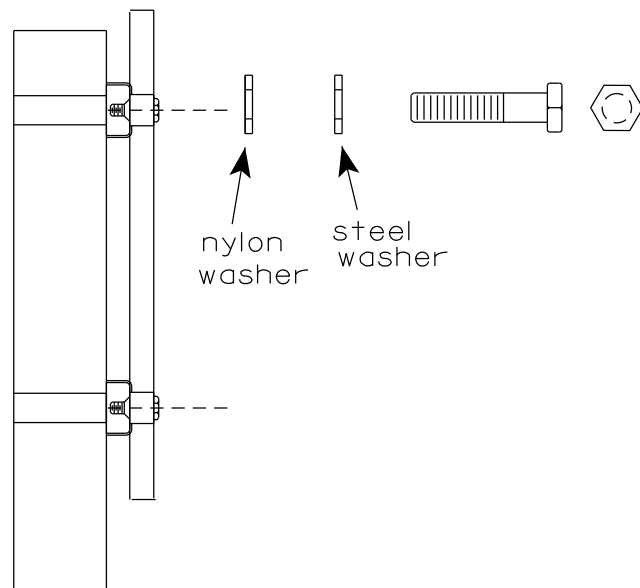


CHANNEL
SEE TYPICAL PANEL
INSTALLATION SHEET



- GENERAL NOTES**
1. Any sign over 3 feet in width shall use the V-Block banding method. See A5-10 standard plate.
 2. Signs 3 feet or greater in height shall have three bracket bands installed. Signs less than 3 feet in height shall have two bracket bands installed.
 3. Banding and assembly bracket shall be stainless steel. All bands shall be $\frac{3}{4}$ " in width and 0.025" thickness.
 4. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
 - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
 - b. Electro-galvanized in accordance with ASTM designation: B 633, Type III, SC 3

WASHER PLACEMENT



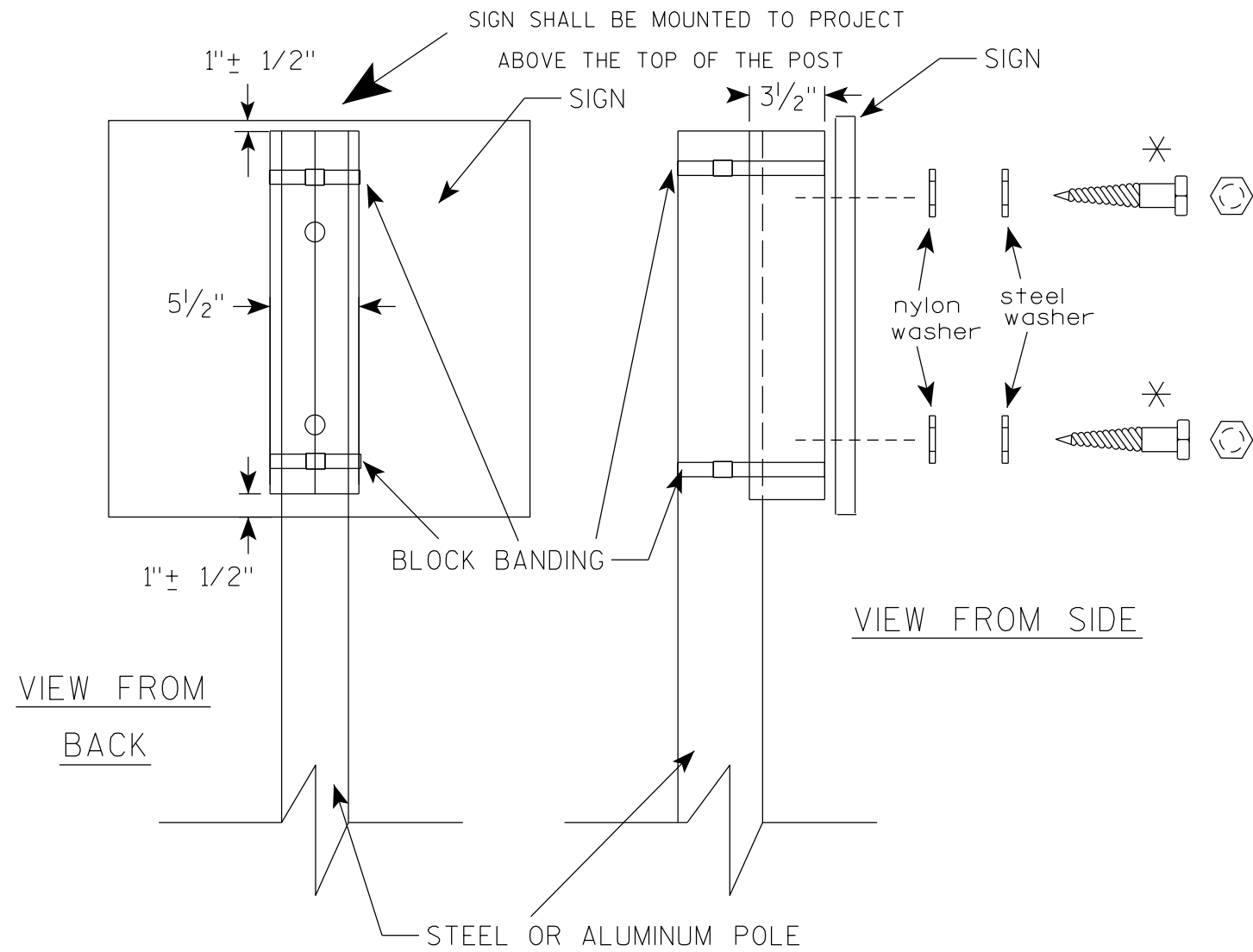
WASHERS (ALL POSTS) -
 1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
 1-1/4" O.D. X 3/8" I.D. X .080 NYLON
 FOR ALL TYPE H SIGNS

STANDARD SIGN
SIGN BANDING DETAILS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

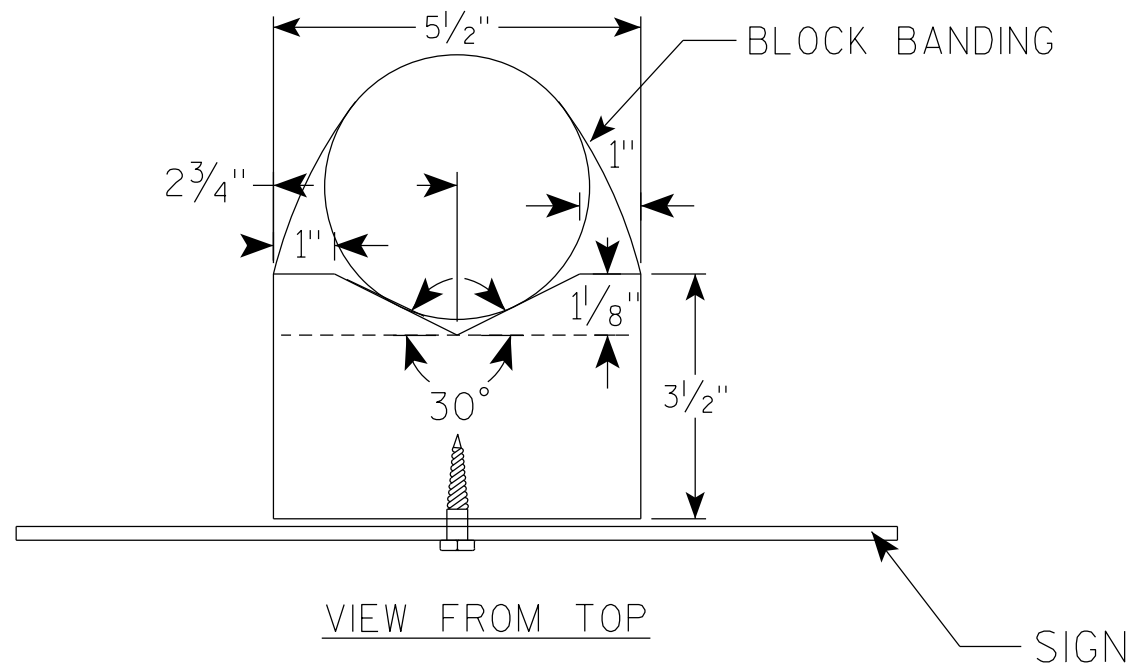
DATE 6/10/19 PLATE NO. A5-9.4



GENERAL NOTES

1. WOOD 4"X6" POST MATERIAL SHALL CONFORM TO 507.2.2 OF THE WISDOT STANDARD SPECIFICATIONS
2. BLOCK BANDING AND CLIPS SHALL BE STAINLESS STEEL, 3/4" WIDTH AND 0.025" THICKNESS
3. SIGNS 3' OR GREATER IN HEIGHT SHALL UTILIZE 3 BLOCK BANDS. SIGNS UNDER 3' IN HEIGHT SHALL UTILIZE 2 BLOCK BANDS
4. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA, BUT NORMALLY THERE ARE TWO. FOR SIGNS GREATER THAN 9 S.F. 3 FASTENERS SHALL BE USED.
5. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
 - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
 - b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3
6. ALL BOLTS SHALL HAVE HEXAGONAL HEADS.
7. STEEL WASHERS SHALL BE 1/4" O.D. X 3/8" I.D. X 1/16"
8. NYLON WASHERS SHALL BE 1/4" O.D. X 3/8" I.D. X .080 FOR TYPE H OR TYPE F FACE SIGN

✱ LAG BOLTS SHALL BE 3/8" X 2 1/2"



BLOCK BANDING DETAIL
(V-BLOCK OPTION)

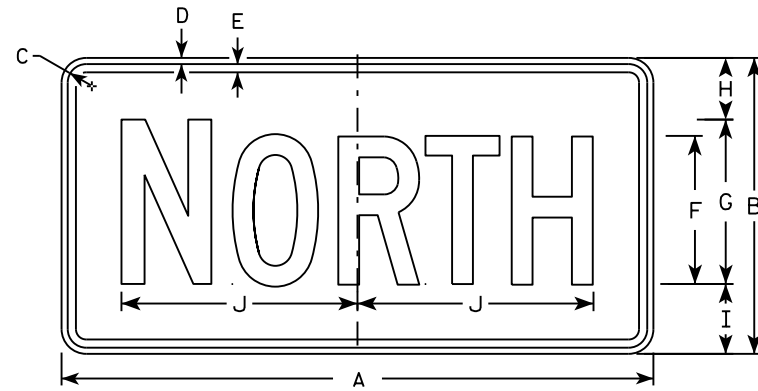
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

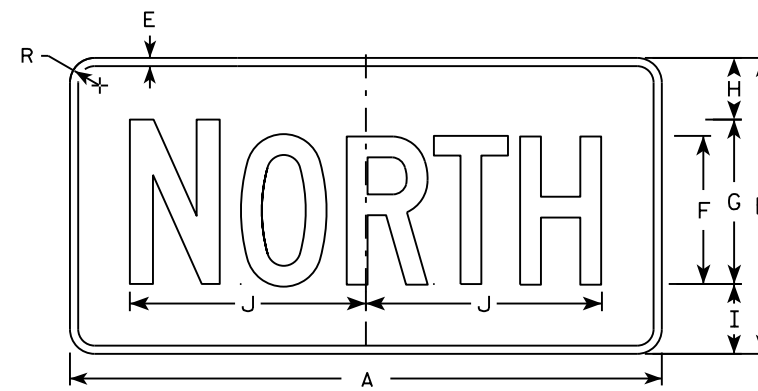
DATE 4/19/2022 PLATE NO. A5-10.3

NOTES

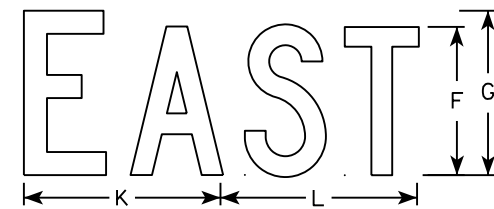
- All Signs Type II - Type H
- Color:
 - Background - See note 5
 - Message - See note 5
- Message Series - C
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- M3-1 thru M3-4 Background - White
 Message - Black
 MB3-1 thru MB3-4 Background - Blue
 Message - White
 MK3-1 thru MK3-4 Background - Green
 Message - White
 MM3-1 thru MM3-4 Background - White
 Message - Green
 MN3-1 thru MN3-4 Background - Brown
 Message - White
 MP3-1 thru MP3-4 Background - White
 Message - Blue
- Note the first letter of each direction is larger than the remainder of the message.



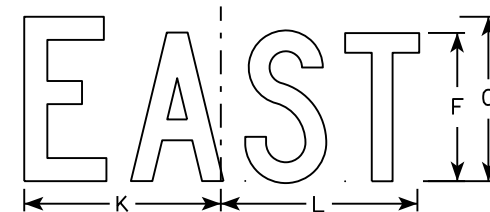
M3-1
MM3-1
MP3-1



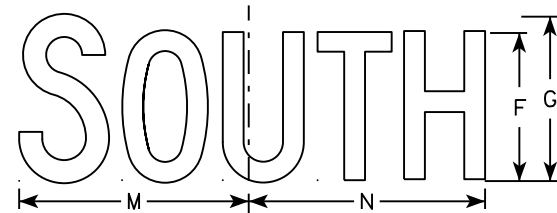
MB3-1
MK3-1
MN3-1



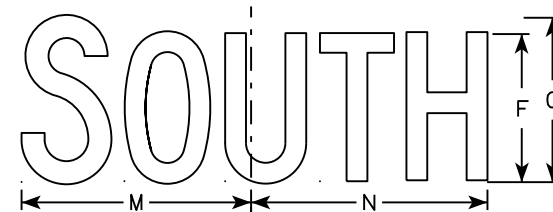
M3-2
MM3-2
MP3-2



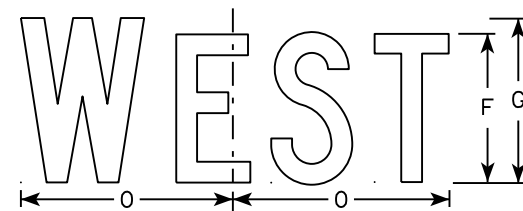
MB3-2
MK3-2
MN3-2



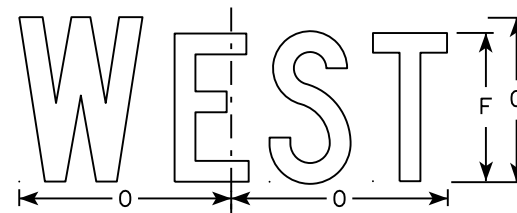
M3-3
MM3-3
MP3-3



MB3-3
MK3-3
MN3-3



M3-4
MM3-4
MP3-4



MB3-4
MK3-4
MN3-4

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	12	1 1/8	3/8	3/8	6	7	2 1/4	2 3/4	10 1/4	7 7/8	8 3/8	10 1/4	9 3/4	8 3/4			1 1/2									2.00
3	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
4	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
5	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5

STANDARD SIGNS
M3-1 thru M3-4
SERIES

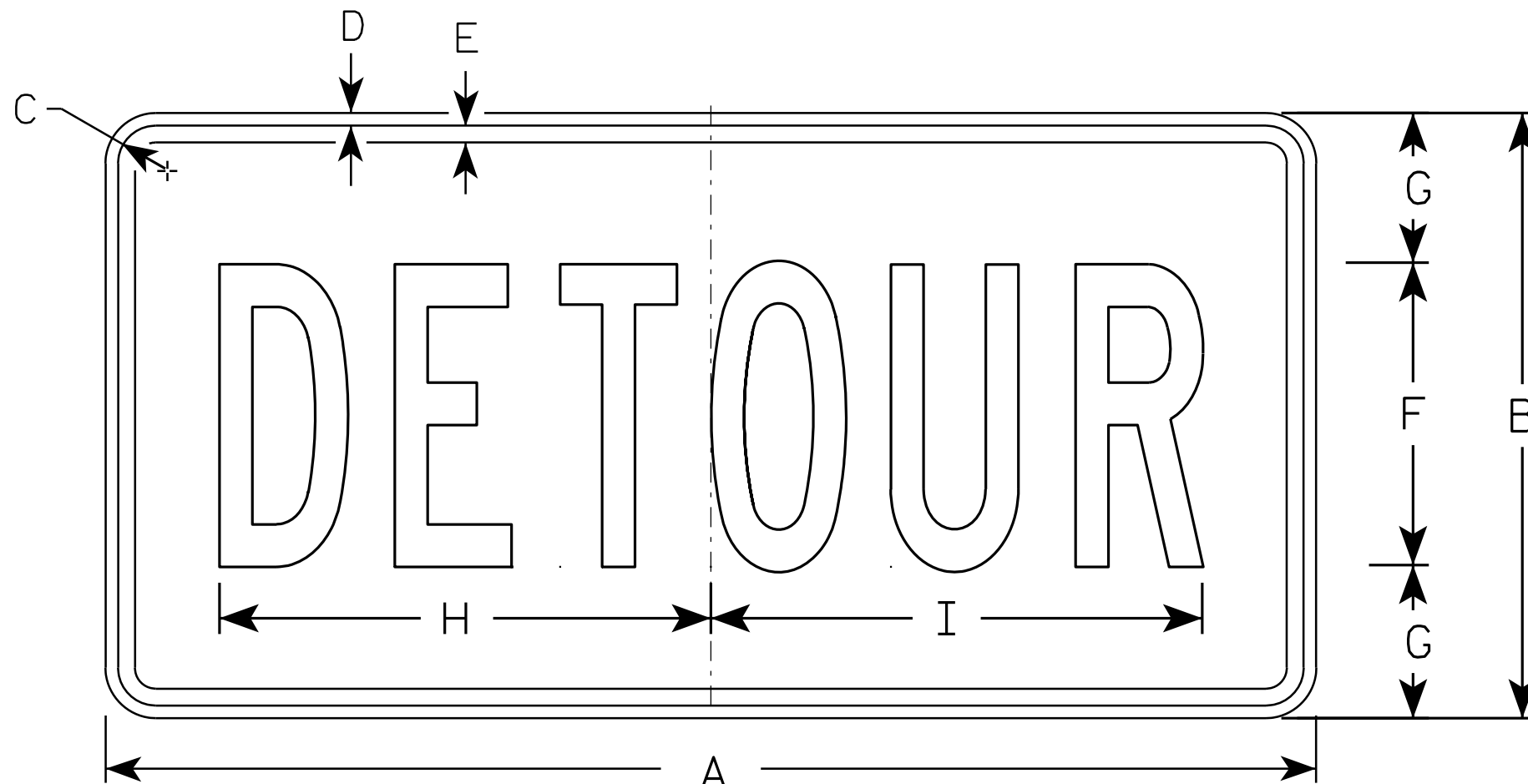
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 10/15/15 PLATE NO. M3-1.14

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - B
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



M4-8

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	12	1 1/8	3/8	3/8	6	3	10	10 1/4																		2.0
3	36	18	1 1/8	3/8	1/2	9	4 1/2	14 5/8	14 1/2																		4.5
4																											
5																											

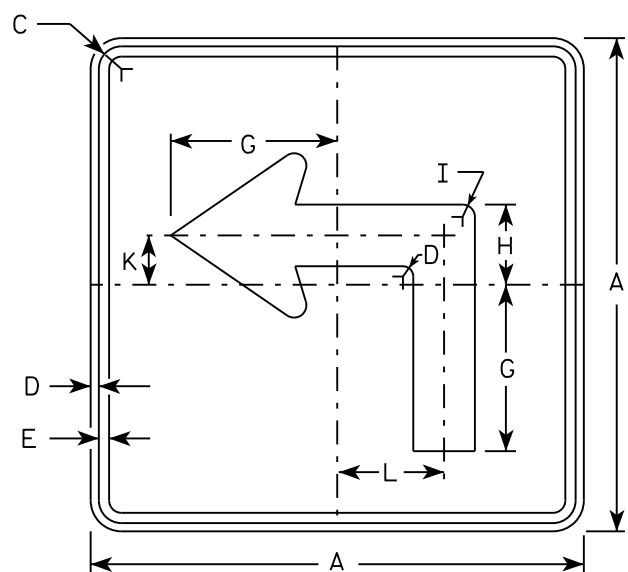
STANDARD SIGN
M4-8

WISCONSIN DEPT OF TRANSPORTATION

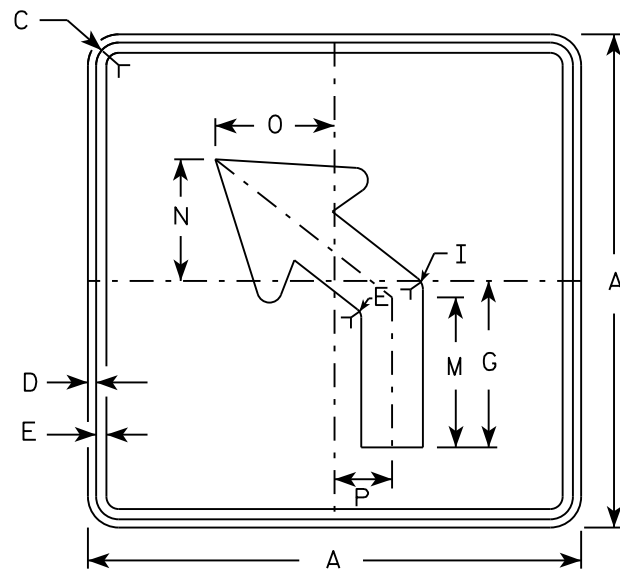
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 11/10/10 PLATE NO. M4-8.2

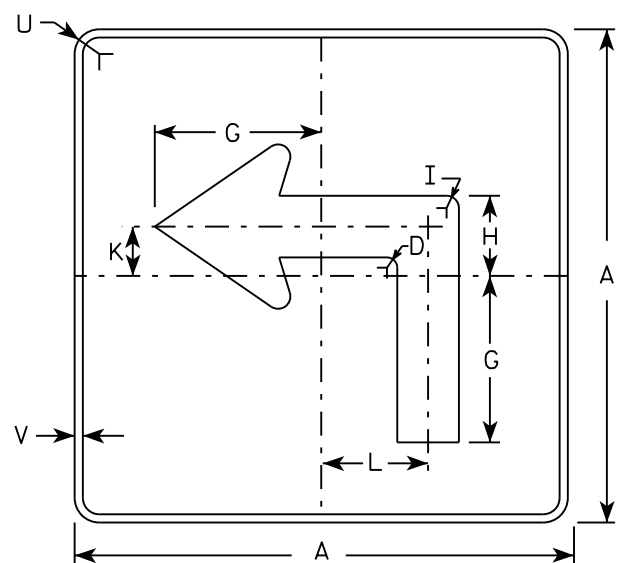
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**



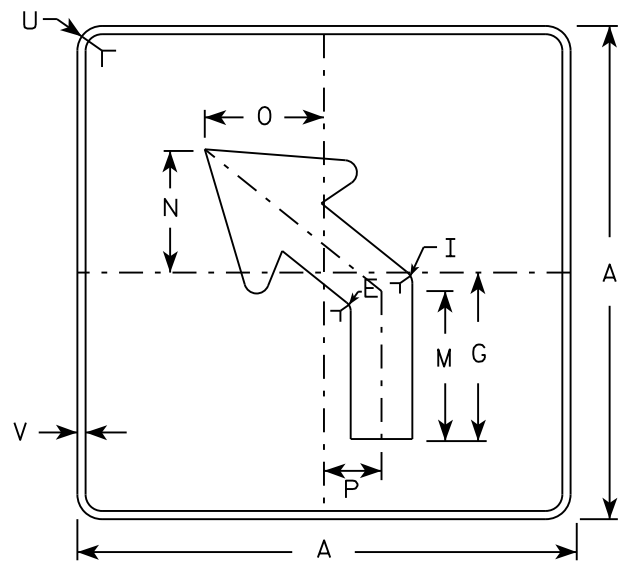
M5-1L
MM5-1L
M05-1L
MP5-1L



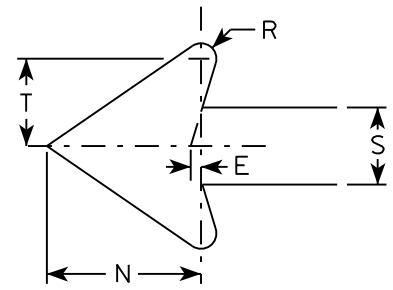
M5-2L
MM5-2L
M05-2L
MP5-2L



MB5-1L
MK5-1L
MN5-1L
MR5-1L



MB5-2L
MK5-2L
MN5-2L
MR5-2L



NOTES

- Signs are Type II - Type H reflective except as shown
- Color:
 - Background - See note 4
 - Message - See note 4
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- M5-1 and M5-2 Background - White
Message - Black
 - MB5-1 and MB5-2 Background - Blue
Message - White
 - MK5-1 and MK5-2 Background - Green
Message - White
 - MM5-1 and MM5-2 Background - White
Message - Green
 - MN5-1 and MN5-2 Background - Brown
Message - White
 - M05-1 and M05-2 Background - Orange - Type F Reflective
Message - Black
 - MP5-1 and MP5-2 Background - White - Type H Reflective
Message - Blue
 - MR5-1 and MR5-2 Background - Brown
Message - Yellow
- M5-1R same as M5-1L except arrow points right.
- M5-2R same as M5-2L except arrow tilts right.

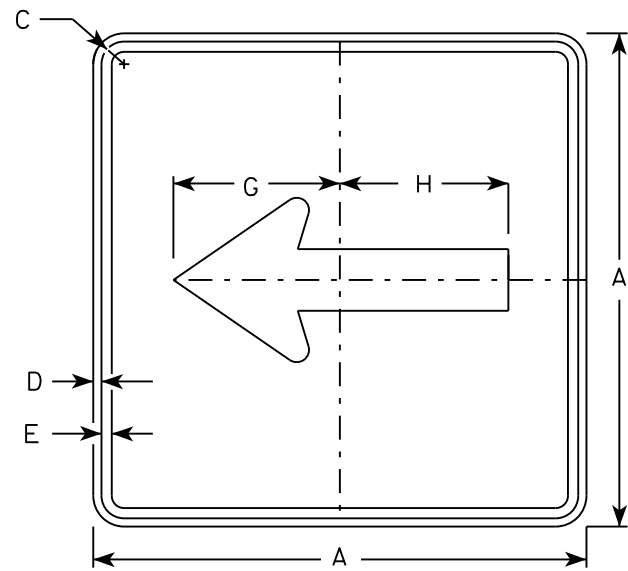
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7	3 3/8	5/8		2 1/8	4 1/2	6 3/8	5 1/4	5	2 1/2		1/2	2 5/8	3	1 1/2	1/2					3.06
3	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25
4	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25
5	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25

STANDARD SIGN
M5-1 & M5-2

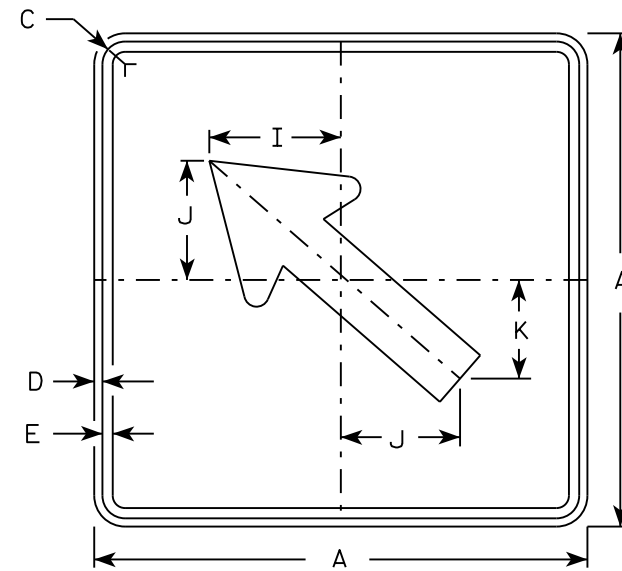
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

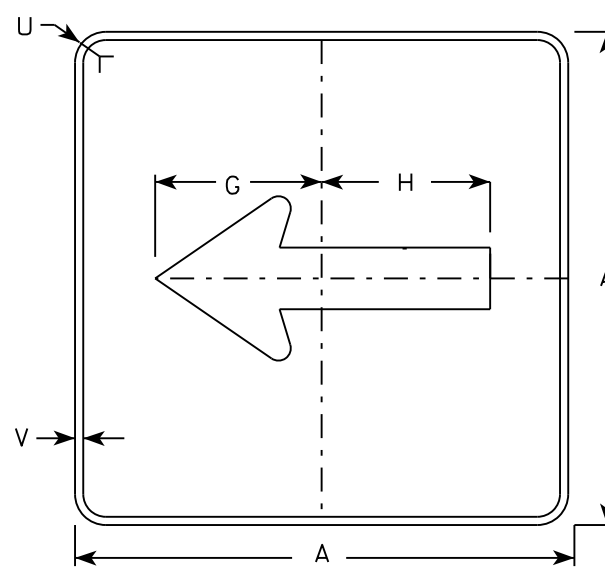
DATE 10/15/15 PLATE NO. M5-1.13



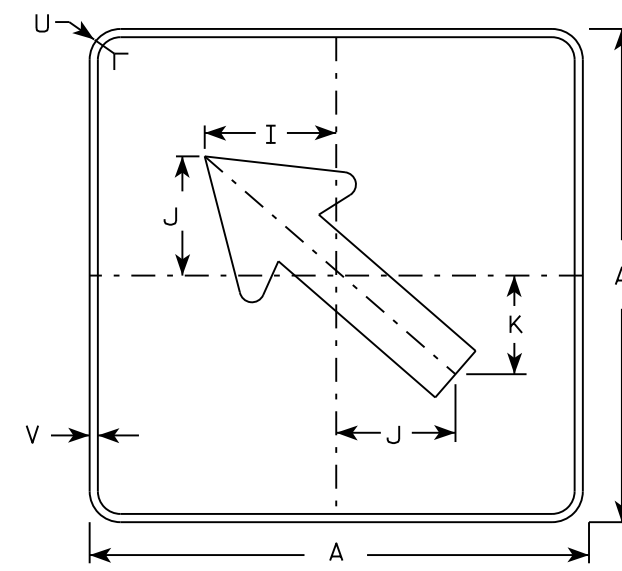
M6-1
MM6-1
M06-1
MP6-1



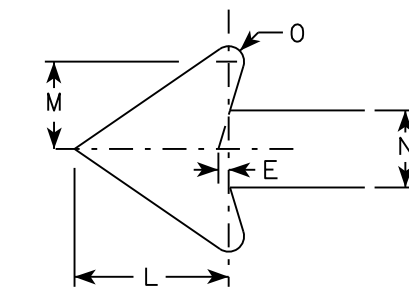
M6-2
MM6-2
M06-2
MP6-2



MB6-1
MK6-1
MN6-1
MR6-1



MB6-2
MK6-2
MN6-2
MR6-2



NOTES

- Signs are Type II - Type H except as Shown
- Color:
Background - See note 4
Message - See note 4
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- M6-1 and M6-2 Background - White
Message - Black
MB6-1 and MB6-2 Background - Blue
Message - White
MK6-1 and MK6-2 Background - Green
Message - White
MM6-1 and MM6-2 Background - White
Message - Green
MN6-1 and MN6-2 Background - Brown
Message - White
M06-1 and M06-2 Background - Orange - Type F Reflective
Message - Black
MP6-1 and MP6-2 Background - White
Message - Blue
MR6-1 and MR6-2 Background - Brown
Message - Yellow

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7 1/2	7 1/8	5 5/8	5	4 1/4	5 1/4	3	2 5/8	1/2						1 1/2	1/2					3.06
3	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25
4	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25
5	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25

STANDARD SIGN
M6-1 & M6-2
SERIES

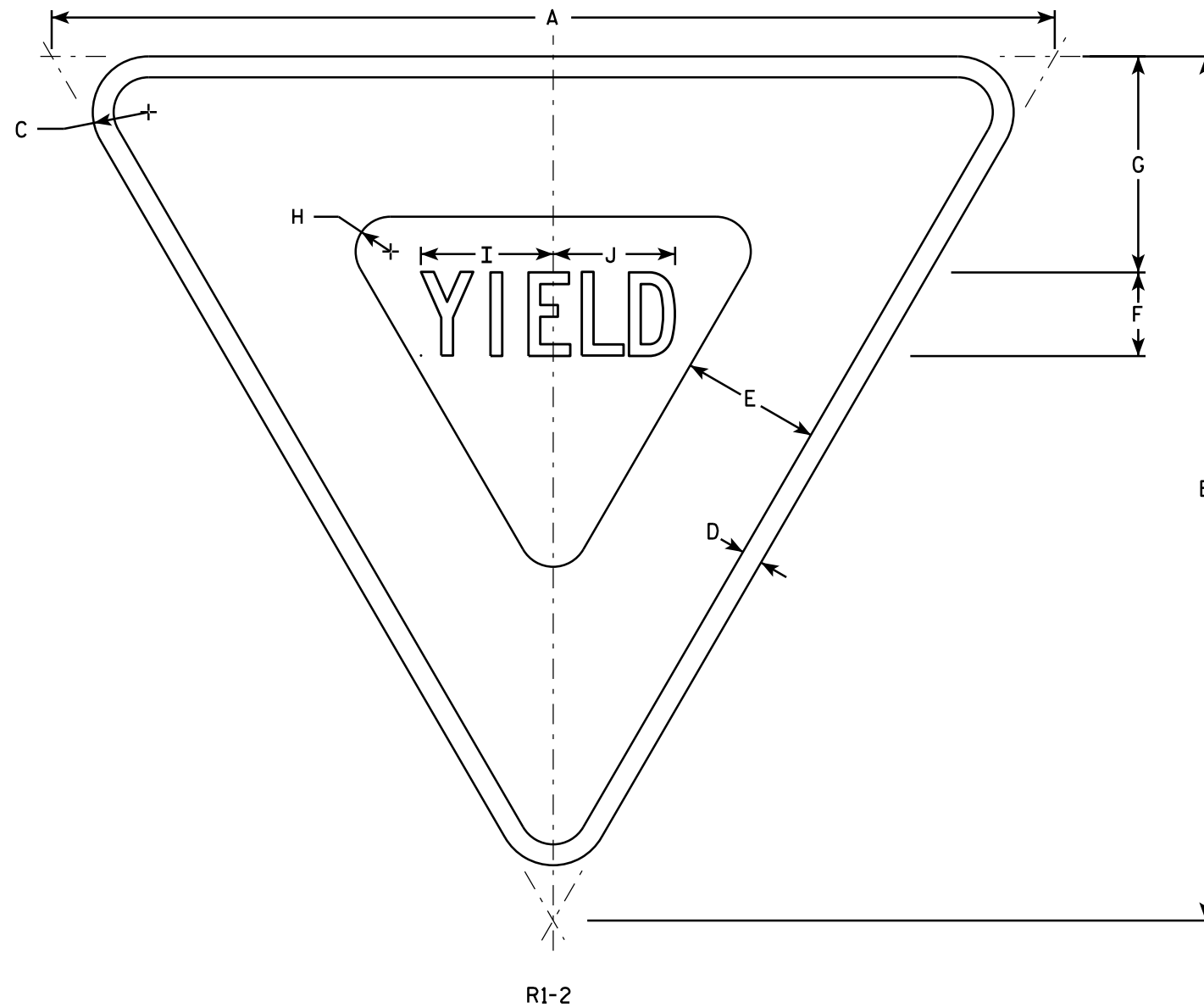
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 10/15/15 PLATE NO. M6-1.15

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - See note 5
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. The border strip and word message are reflectorized red.



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30	26	1 1/2	5/8	4	2 1/2	6 3/8	7/8	4	3 5/8																	2.71
2S	36	31	2	3/4	5	3	7 3/4	1 1/4	4 3/4	4 3/8																	3.88
2M	48	42	3	1	6	4	9 3/4	2	6 1/4	5 7/8																	7.00
3	48	42	3	1	6	4	9 3/4	2	6 1/4	5 7/8																	7.00
4	48	42	3	1	6	4	9 3/4	2	6 1/4	5 7/8																	7.00
5	60	52	3	1 1/2	8	5	13	2 1/2	7 7/8	7 1/4																	10.83
6																											
7	18	15 1/2	1	3/8	2 1/2	1 1/2	3 7/8	5/8	2 3/8	2 1/4																	0.97

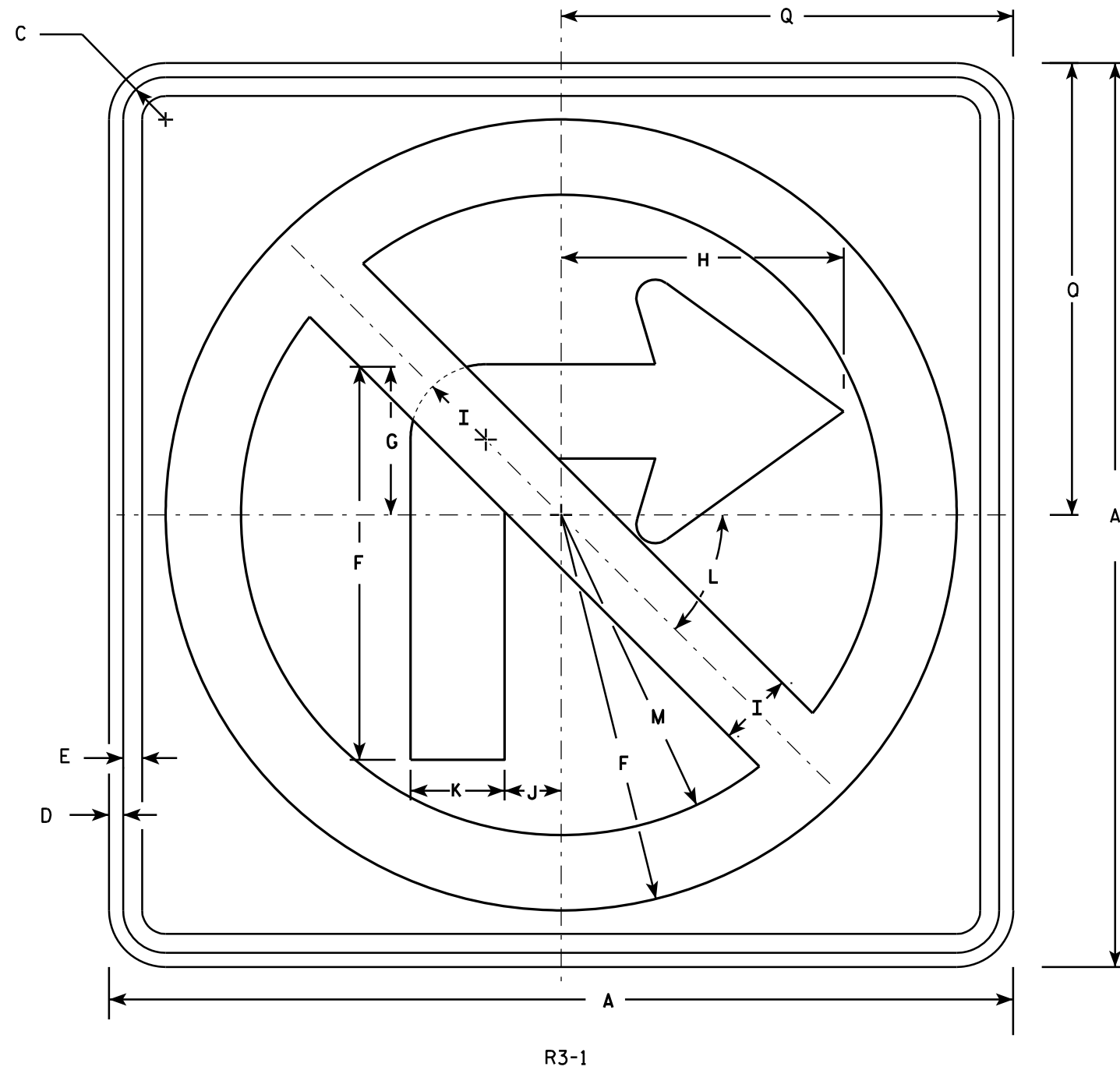
STANDARD SIGN
R1-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

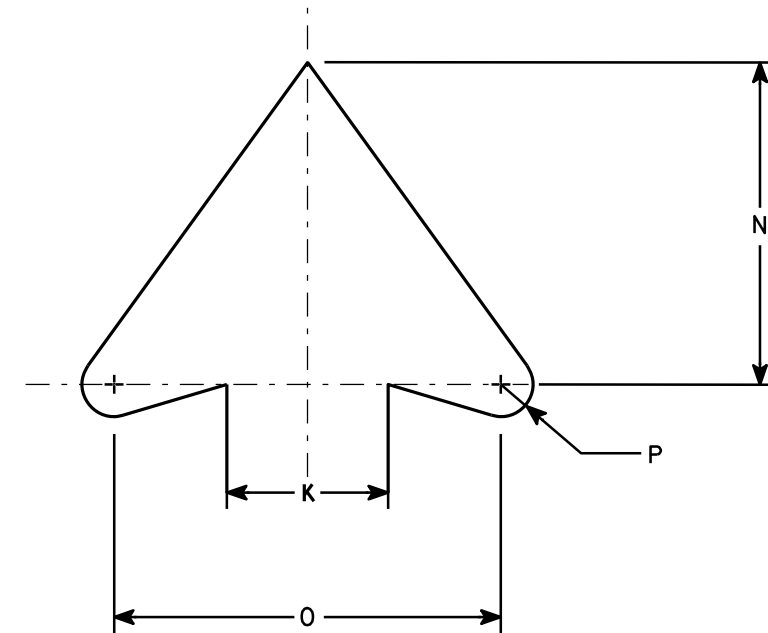
DATE 10/13/14 PLATE NO. R1-2.12

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E



NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - See note 4
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. Border & Arrow are non reflective black, the circle with diagonal bar is reflective red.



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2	10 1/2	4	7 1/2	2	1 1/2	2 1/2	45	8 1/2	5	6	1/2	12										4.0
2S	24		1 1/8	3/8	1/2	10 1/2	4	7 1/2	2	1 1/2	2 1/2	45°	8 1/2	5	6	1/2	12										4.0
2M	36		1 5/8	5/8	3/4	15 3/4	6	11 1/4	3	2 1/4	3 3/4	45	12 3/4	7 1/2	9	3/4	18										9.0
3	36		1 5/8	5/8	3/4	15 3/4	6	11 1/4	3	2 1/4	3 3/4	45	12 3/4	7 1/2	9	3/4	18										9.0
4	36		1 5/8	5/8	3/4	15 3/4	6	11 1/4	3	2 1/4	3 3/4	45°	12 3/4	7 1/2	9	3/4	18										9.0
5	48		2 1/4	3/4	1	21	8	15	4	3	5	45°	17	10	12	1	24										16.0

STANDARD SIGN

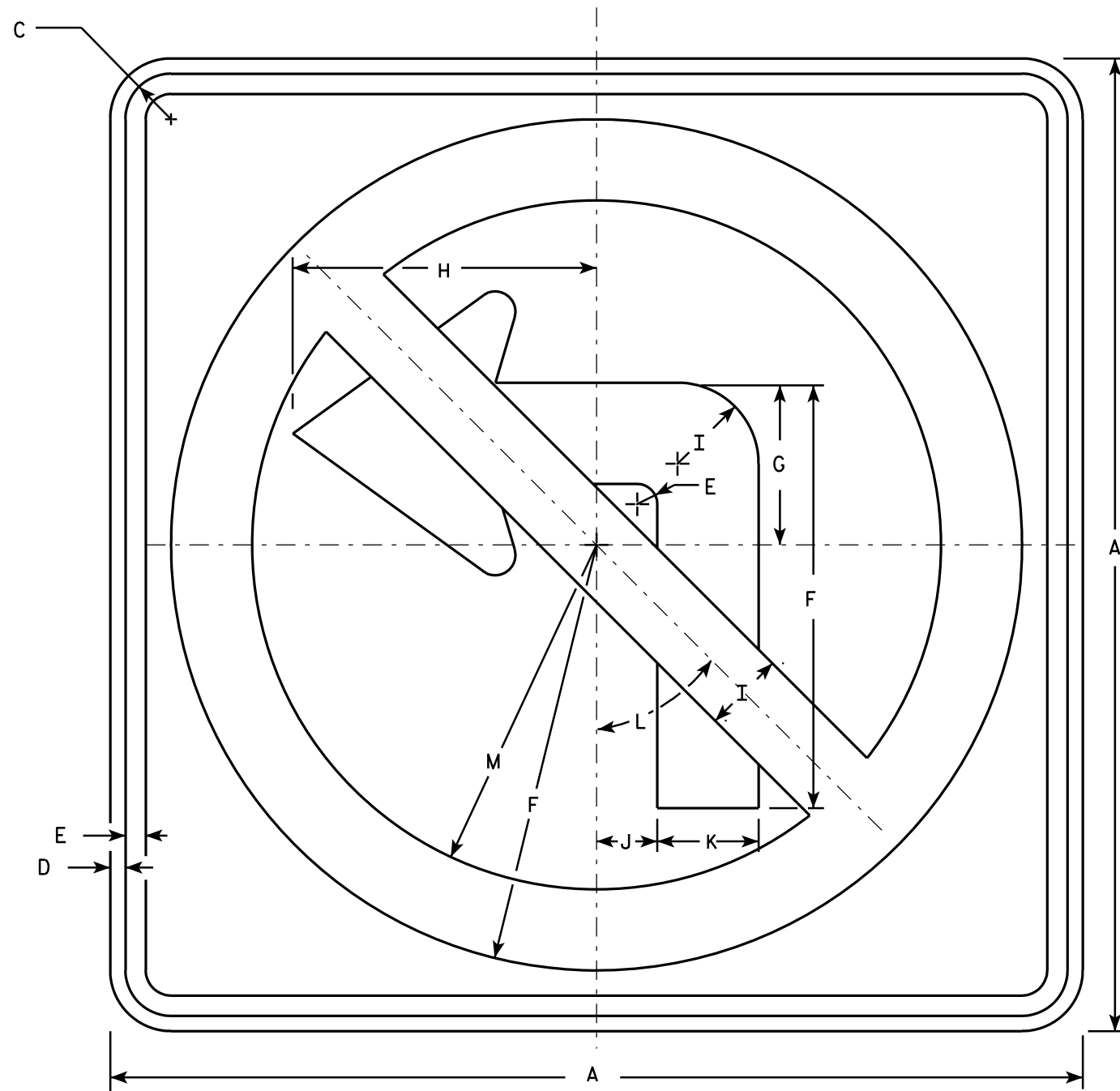
R3-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 12/08/10 PLATE NO. R3-1.5

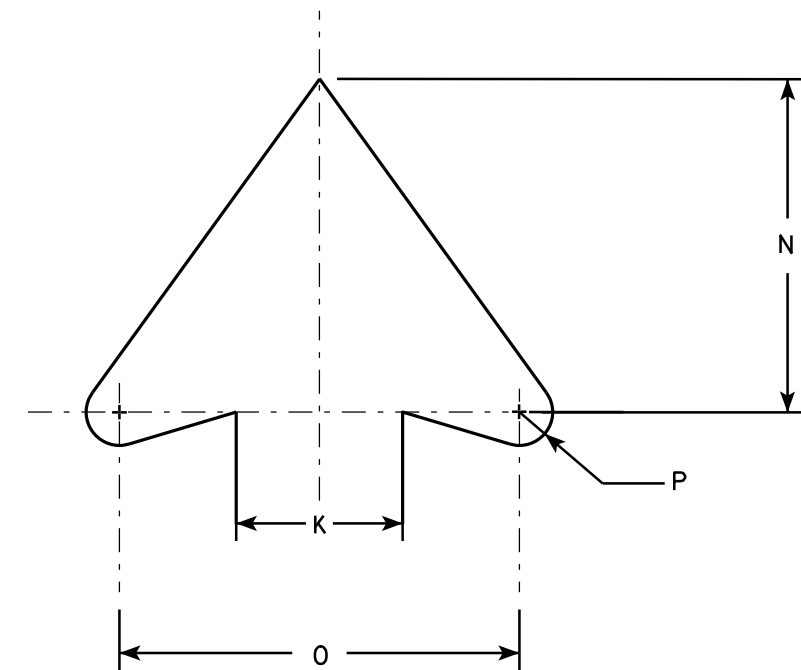
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E



R3-2

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - See note 4
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. Border & Arrow are non reflective black, the circle with diagonal bar is reflective red.



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. Ft.
1	24		1 1/8	3/8	1/2	10 1/2	4	7 1/2	2	1 1/2	2 1/2	45°	8 1/2	5	6	1/2											4.0
2S	24		1 1/8	3/8	1/2	10 1/2	4	7 1/2	2	1 1/2	2 1/2	45°	8 1/2	5	6	1/2											4.0
2M	36		1 5/8	5/8	3/4	15 3/4	6	11 1/4	3	2 1/4	3 3/4	45°	12 3/4	7 1/2	9	3/4											9.0
3	36		1 5/8	5/8	3/4	15 3/4	6	11 1/4	3	2 1/4	3 3/4	45°	12 3/4	7 1/2	9	3/4											9.0
4	36		1 5/8	5/8	3/4	15 3/4	6	11 1/4	3	2 1/4	3 3/4	45°	12 3/4	7 1/2	9	3/4											9.0
5	48		2 1/4	3/4	1	21	8	15	4	3	5	45°	17	10	12	1											16.0

STANDARD SIGN
R3-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 12/08/10 PLATE NO. R3-2.10

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E

NOTES

1. Sigs are Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - D
4. Use appropriate Letter for Sign Code
Each letter added makes sign wider. Example R3-8EAR
5. Square footage of sign varies by letters

1 Letter = 3.75 sq ft for Size 2
6.0 sq ft for Size 3
10.0 sq ft for Size 4 or 5

2 Letters = 7.5 sq ft for Size 2
12.0 sq ft for Size 3
20.0 sq ft for Size 4 or 5

3 Letters = 11.25 sq ft for Size 2
18.0 sq ft for Size 3
30.0 sq ft for Size 4 or 5

4 Letters = 15.0 sq ft for Size 2
24.0 sq ft for Size 3
40.0 sq ft for Size 4 or 5

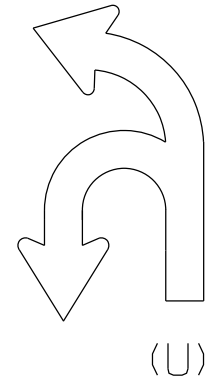
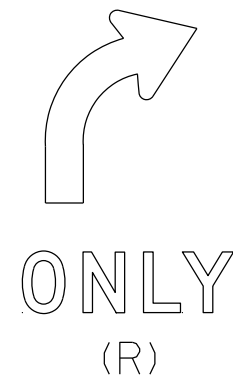
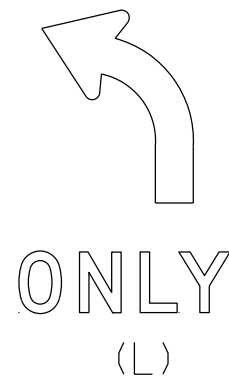
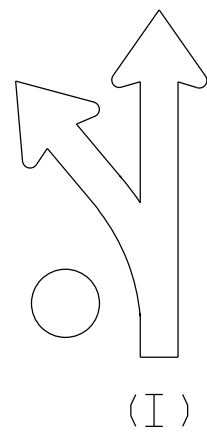
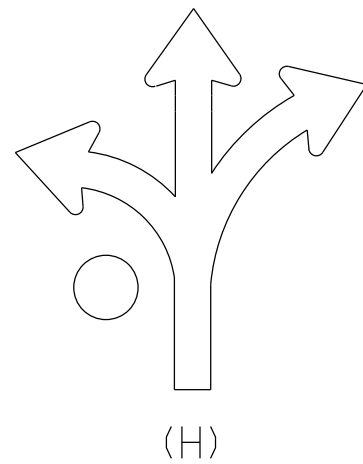
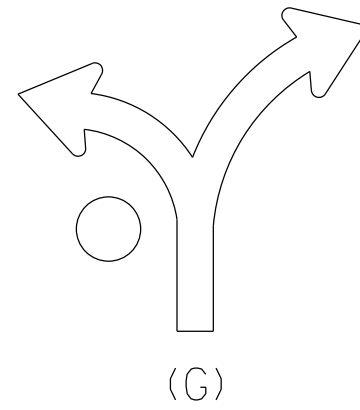
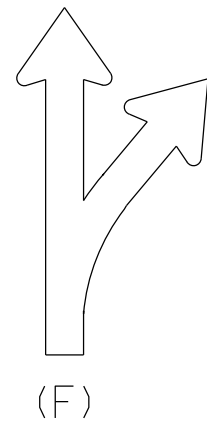
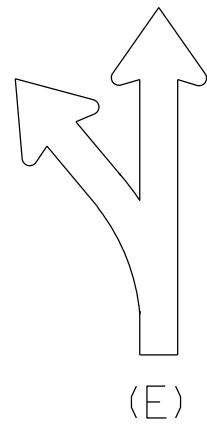
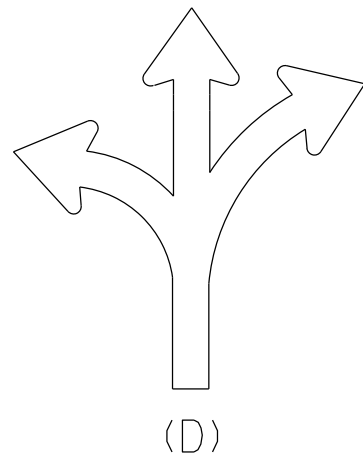
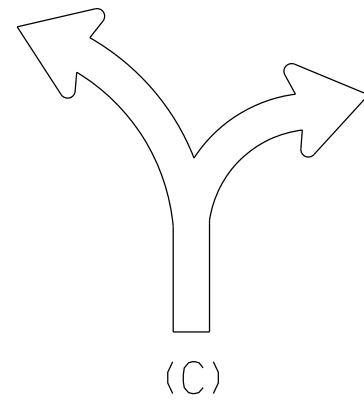
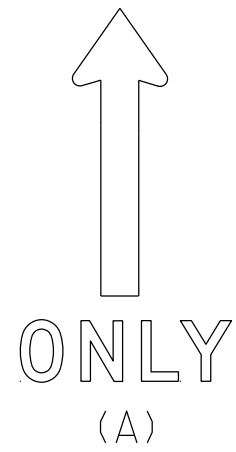
5 Letters = 18.75 sq ft for Size 2
30.0 sq ft for Size 3
50.0 sq ft for Size 4 or 5

6 Letters = 22.5 sq ft for Size 2
36.0 sq ft for Size 3
60.0 sq ft for Size 4 or 5

6. When letters C,D,G,H are used on the Left or Right end of the sign the Sq.Ft. changes.

Add the amounts when these letters are used:

1.25 sq ft for Size 2
1.5 sq ft for Size 3
2.0 sq ft for Size 4 or 5



STANDARD SIGN
R3-8 Series

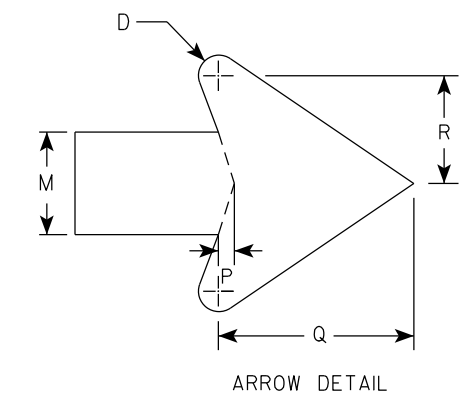
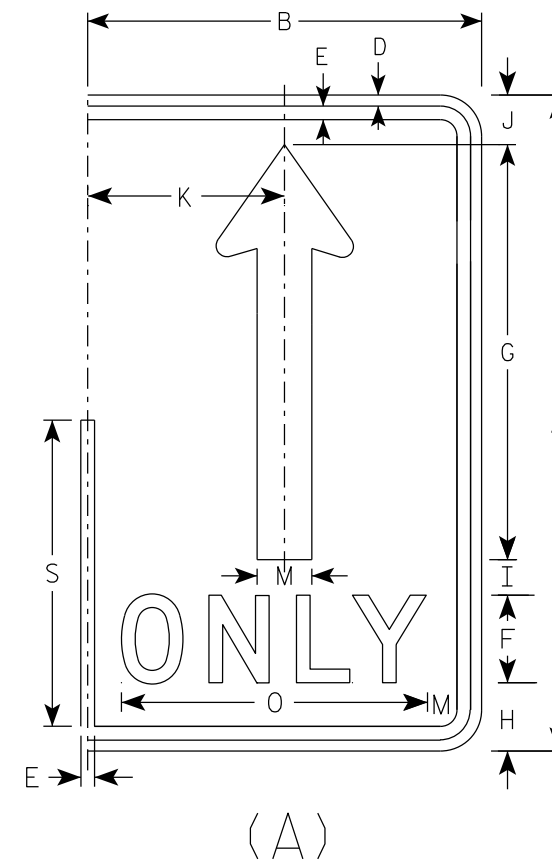
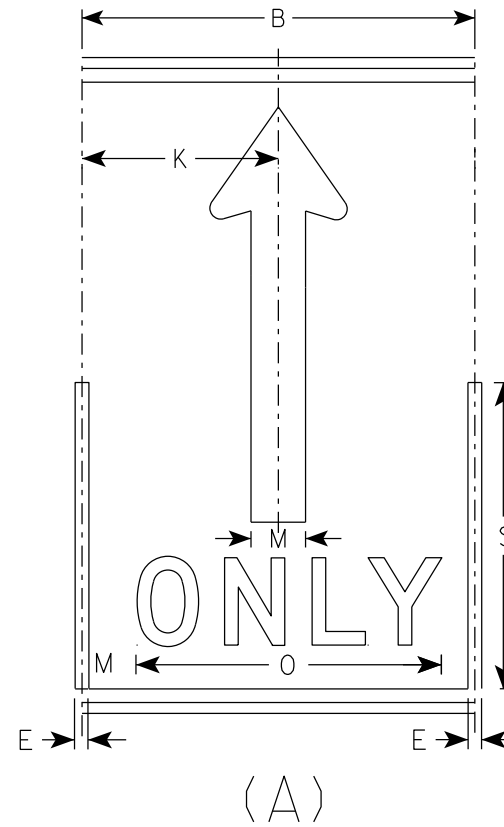
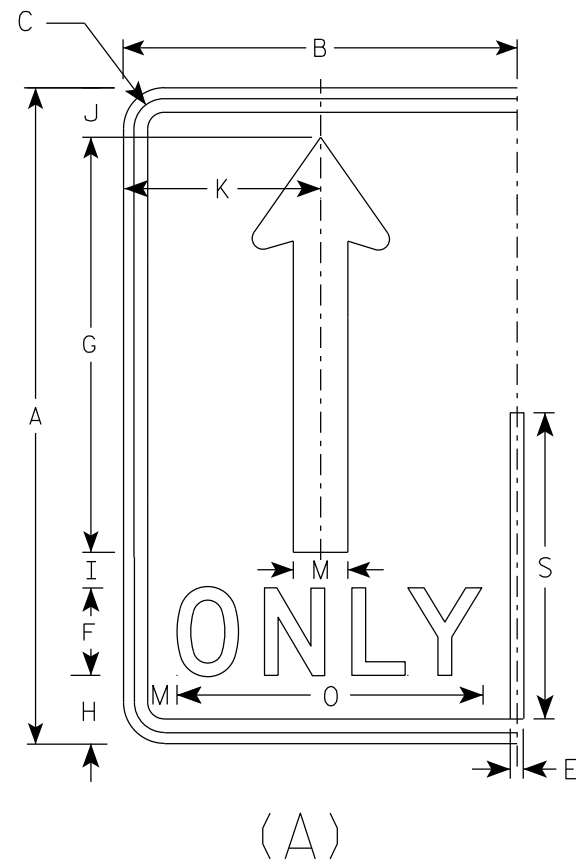
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 5/21/19 PLATE NO. R3-8.1

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - D



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30	18	1 3/8	1/2	5/8	4	19	3 1/8	1 5/8	2 1/4	9		2 1/2		14	3/8	4 3/4	2 5/8	14								3.75
2M	30	18	1 3/8	1/2	5/8	4	19	3 1/8	1 5/8	2 1/4	9		2 1/2		14	3/8	4 3/4	2 5/8	14								3.75
3	36	24	1 3/8	1/2	5/8	5	22 3/4	3 3/4	1 3/4	2 3/4	12		3		17 5/8	1/2	5 3/4	3 1/8	16 3/4								6.0
4	48	30	2 1/4	3/4	1	6	30 3/8	5 1/8	2 7/8	3 5/8	15		4		21 3/4	5/8	7 5/8	4 1/4	22 3/8								10.0
5	48	30	2 1/4	3/4	1	6	30 3/8	5 1/8	2 7/8	3 5/8	15		4		21 3/4	5/8	7 5/8	4 1/4	22 3/8								10.0

STANDARD SIGN
R3-8 (A) Arrow

WISCONSIN DEPT OF TRANSPORTATION

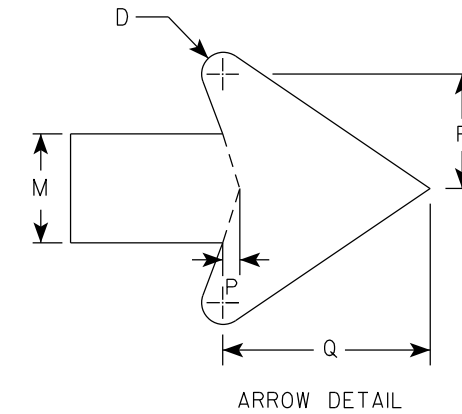
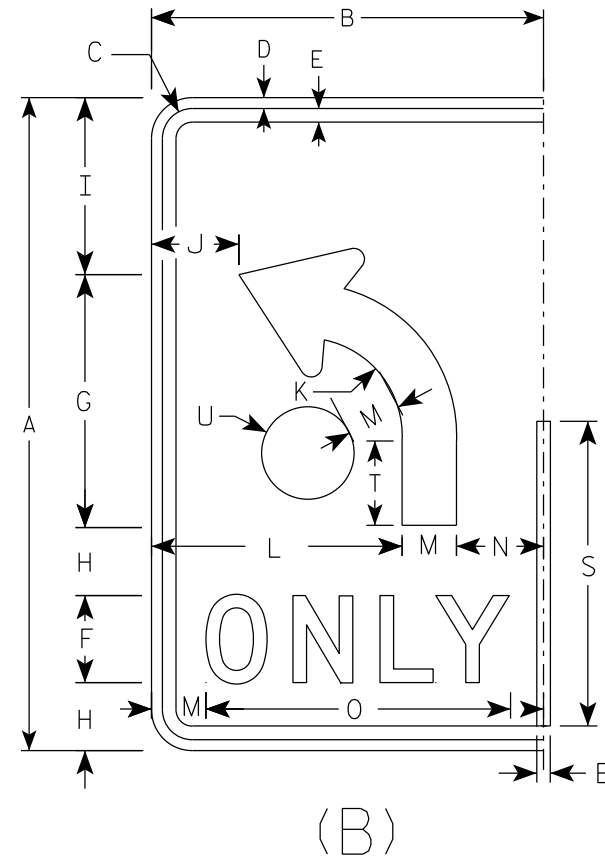
APPROVED *Matthew R Rauch*
for State Traffic Engineer

DATE 5/21/19 PLATE NO. R3-8.1

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
 - Background - White
 - Message - Black
 - Message Series - D



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30	18	1 3/8	1/2	5/8	4	11 5/8	3 1/8	8 1/8	4	4 1/2	11 1/2	2 1/2	4	14	3/8	4 3/4	2 5/8	14	3 7/8	2 1/8						3.75
2M	30	18	1 3/8	1/2	5/8	4	11 5/8	3 1/8	8 1/8	4	4 1/2	11 1/2	2 1/2	4	14	3/8	4 3/4	2 5/8	14	3 7/8	2 1/8						3.75
3	36	24	1 3/8	1/2	5/8	5	14	3 1/2	9 3/4	6	5 3/8	15	3	6	17 5/8	1/2	5 3/4	3 1/8	16 3/4	4 5/8	2 1/2						6.0
4	48	30	2 1/4	3/4	1	6	18 5/8	5 1/8	13 1/8	6 1/8	7 1/4	18	4	8	21 3/4	5/8	7 5/8	4 1/4	22 3/8	6 1/4	3 3/8						10.0
5	48	30	2 1/4	3/4	1	6	18 5/8	5 1/8	13 1/8	6 1/8	7 1/4	18	4	8	21 3/4	5/8	7 5/8	4 1/4	22 3/8	6 1/4	3 3/8						10.0

STANDARD SIGN
R3-8 (B) Arrow

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 5/21/19 PLATE NO. R3-8.1

PROJECT NO:

SHEET NO:

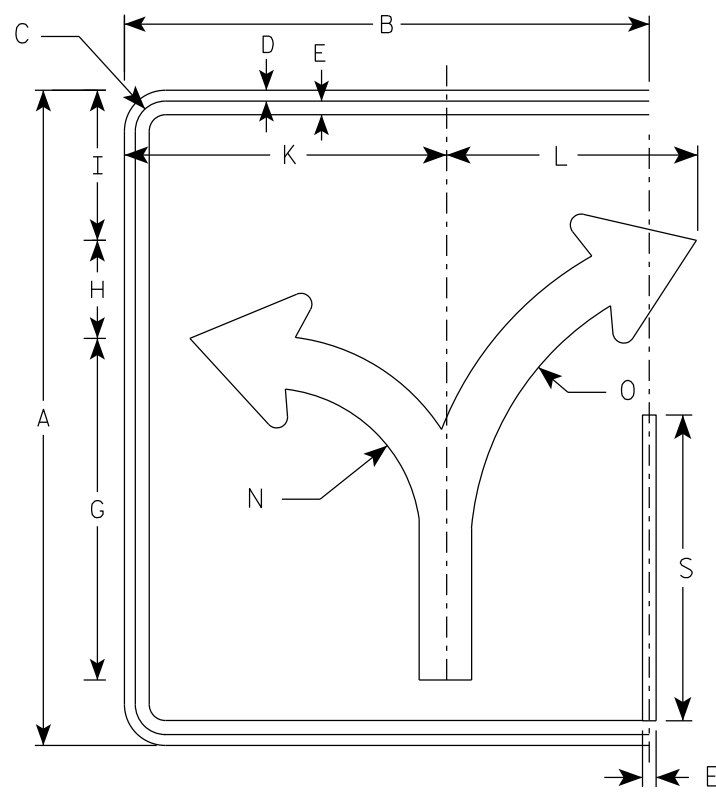
E

7

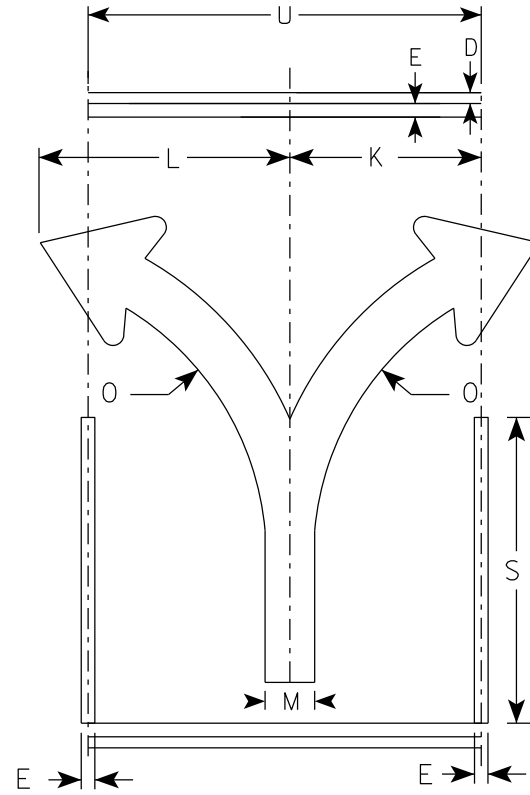
7

NOTES

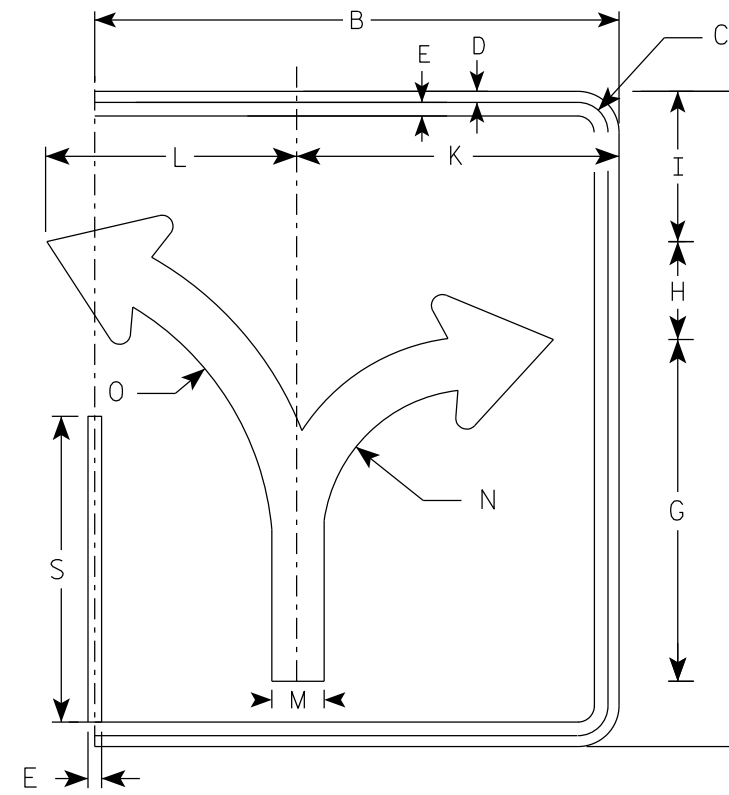
1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - None



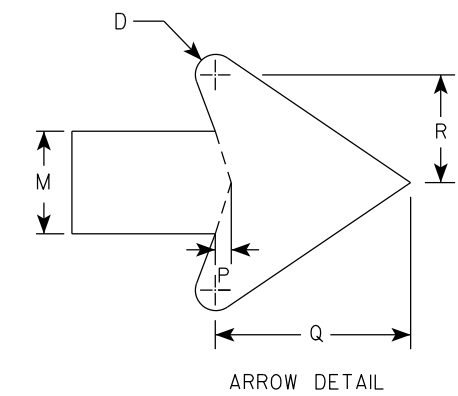
(C)



(C)



(C)



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	ENDS	MIDDLE
																											Area sq. ft.	Area sq. ft.
1																												
2S	30	24	1 3/8	1/2	5/8		15 5/8	4 1/2	6 7/8		14 3/4	11 1/2	2 3/8	7	13 1/4	3/8	4 1/2	2 1/2	14		18						5.0	3.75
2M	30	24	1 3/8	1/2	5/8		15 5/8	4 1/2	6 7/8		14 3/4	11 1/2	2 3/8	7	13 1/4	3/8	4 1/2	2 1/2	14		18						5.0	3.75
3	36	30	1 3/8	1/2	5/8		18 3/4	5 1/2	8 1/4		17 1/4	17 1/4	2 7/8	8 3/8	16	1/2	5 1/2	3	16 3/4		24						7.5	6.0
4	48	36	2 1/4	3/4	1		24 7/8	7 1/4	11		23 1/8	18	3 3/4	11 1/8	21 1/4	5/8	7 1/8	4	22 3/8		30						12.0	10.0
5	48	36	2 1/4	3/4	1		24 7/8	7 1/4	11		23 1/8	18	3 3/4	11 1/8	21 1/4	5/8	7 1/8	4	22 3/8		30						12.0	10.0

STANDARD SIGN
R3-8 (C) Arrow

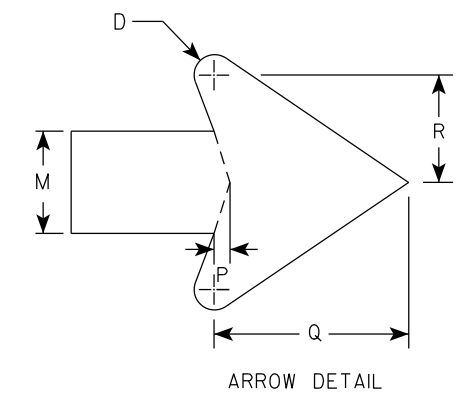
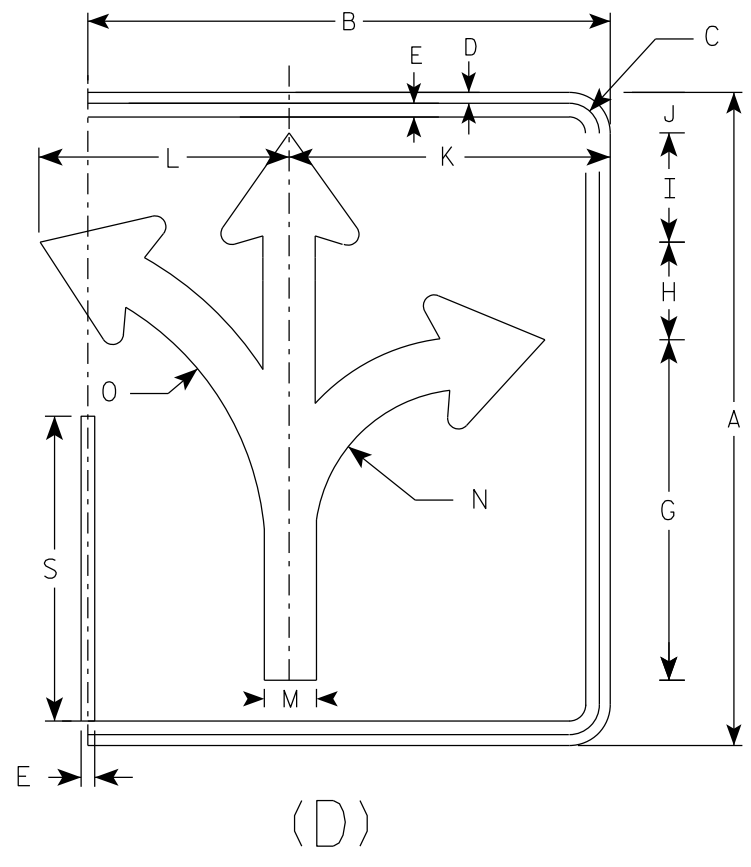
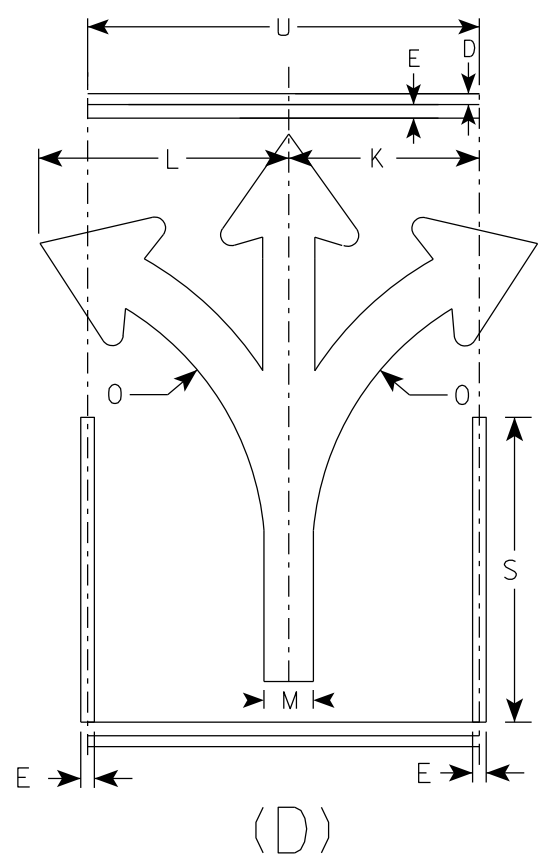
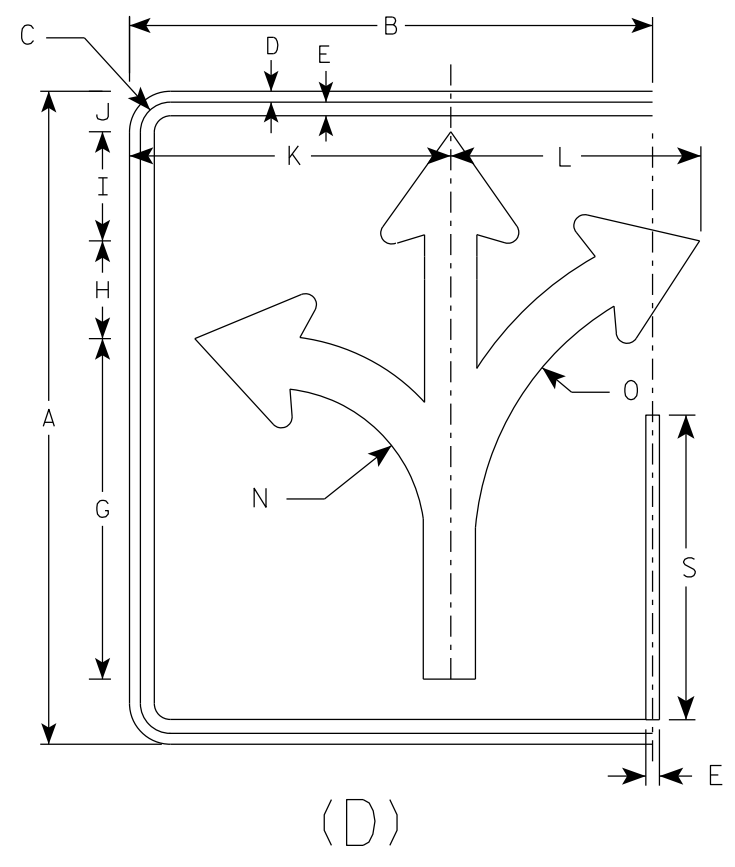
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Rauch*
for State Traffic Engineer

DATE 5/21/19 PLATE NO. R3-8.1

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - None



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	ENDS	MIDDLE
																											Area sq. ft.	Area sq. ft.
1																												
2S	30	24	1 3/8	1/2	5/8		15 5/8	4 1/2	5	1 7/8	14 3/4	11 1/2	2 3/8	7	13 1/4	3/8	4 1/2	2 1/2	14		18						5.0	3.75
2M	30	24	1 3/8	1/2	5/8		15 5/8	4 1/2	5	1 7/8	14 3/4	11 1/2	2 3/8	7	13 1/4	3/8	4 1/2	2 1/2	14		18					5.0	3.75	
3	36	30	1 3/8	1/2	5/8		18 3/4	5 1/2	6	2 1/4	17 1/4	17 1/4	2 7/8	8 3/8	16	1/2	5 1/2	3	16 3/4		24					7.5	6.0	
4	48	36	2 1/4	3/4	1		24 7/8	7 1/4	7 7/8	3 1/8	23 1/8	18	3 3/4	11 1/8	21 1/4	5/8	7 1/8	4	22 3/8		30					12.0	10.0	
5	48	36	2 1/4	3/4	1		24 7/8	7 1/4	7 7/8	3 1/8	23 1/8	18	3 3/4	11 1/8	21 1/4	5/8	7 1/8	4	22 3/8		30					12.0	10.0	

STANDARD SIGN
R3-8 (D) Arrow

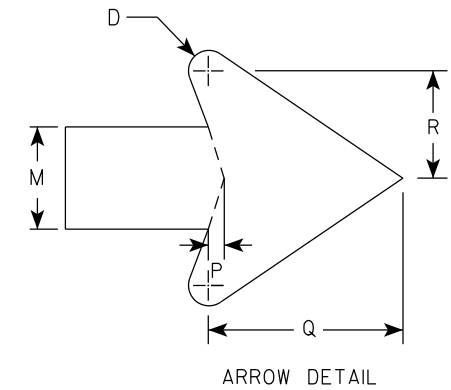
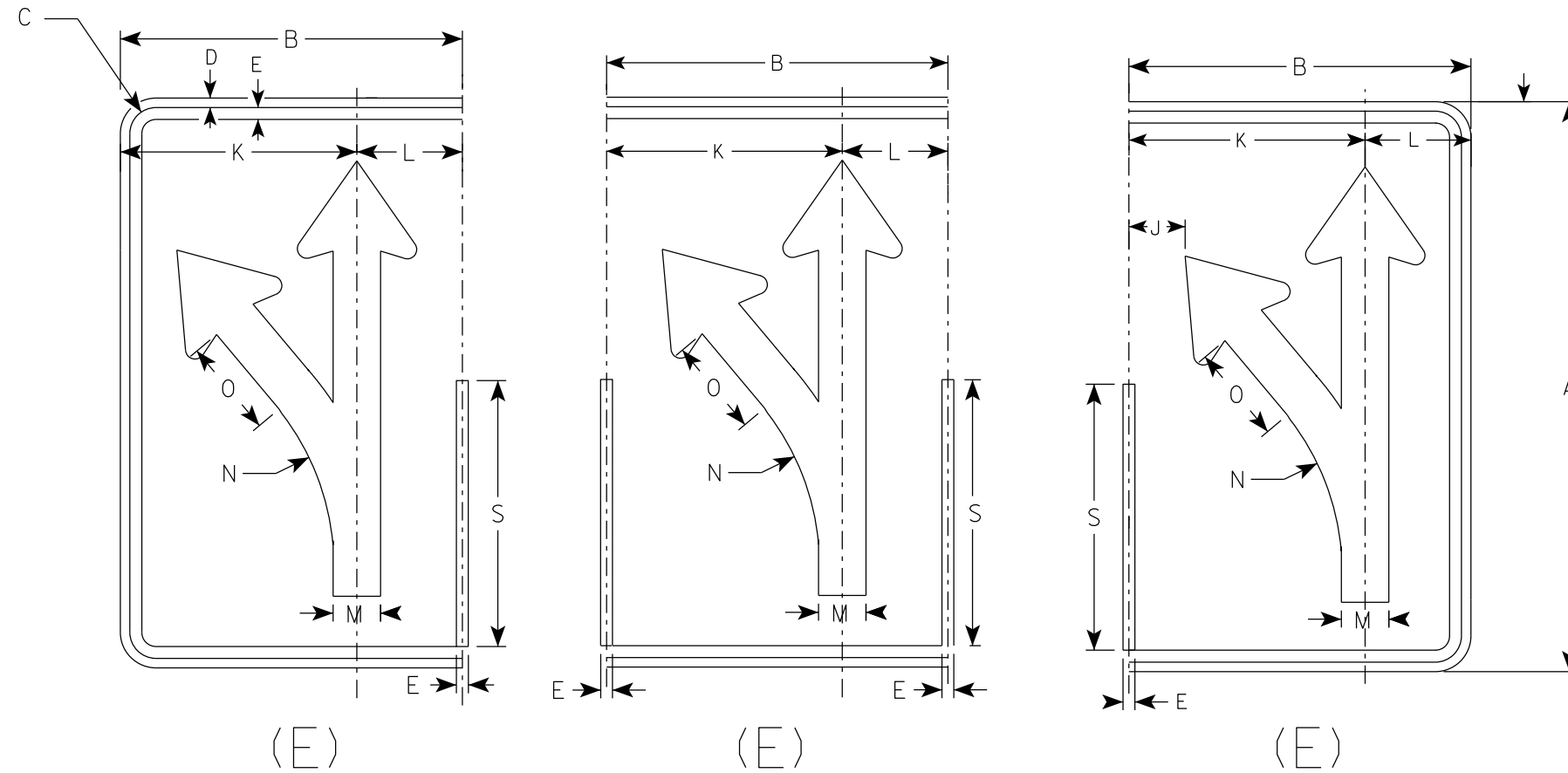
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 5/21/19 PLATE NO. R3-8.1

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
 - Background - White
 - Message - Black
3. Message Series - None



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30	18	1 3/8	1/2	5/8		18 1/4	4 3/4	3 1/4	3	12 1/2	5 1/2	2 1/2	13 1/4	5 1/8	3/8	4 3/4	2 5/8	14								3.75
2M	30	18	1 3/8	1/2	5/8		18 1/4	4 3/4	3 1/4	3	12 1/2	5 1/2	2 1/2	13 1/4	5 1/8	3/8	4 3/4	2 5/8	14								3.75
3	36	24	1 3/8	1/2	5/8		21 7/8	5 5/8	4	4 7/8	16 1/8	7 3/4	3	15 7/8	6 1/8	1/2	5 3/4	3 1/8	16 3/4								6.0
4	48	30	2 1/4	3/4	1		29 1/8	7 1/2	5 1/4	5 3/8	20 1/2	9 1/2	4	21 1/4	8 1/4	5/8	7 5/8	4 1/4	22 3/8								10.0
5	48	30	2 1/4	3/4	1		29 1/8	7 1/2	5 1/4	5 3/8	20 1/2	9 1/2	4	21 1/4	8 1/4	5/8	7 5/8	4 1/4	22 3/8								10.0

STANDARD SIGN
R3-8 (E) Arrow

WISCONSIN DEPT OF TRANSPORTATION

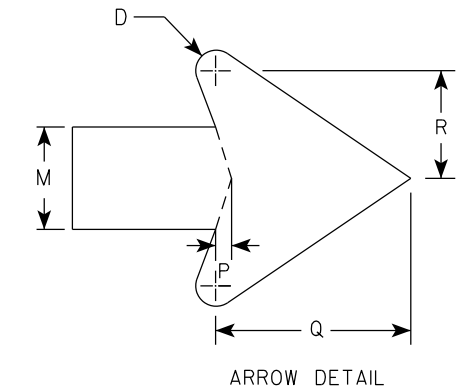
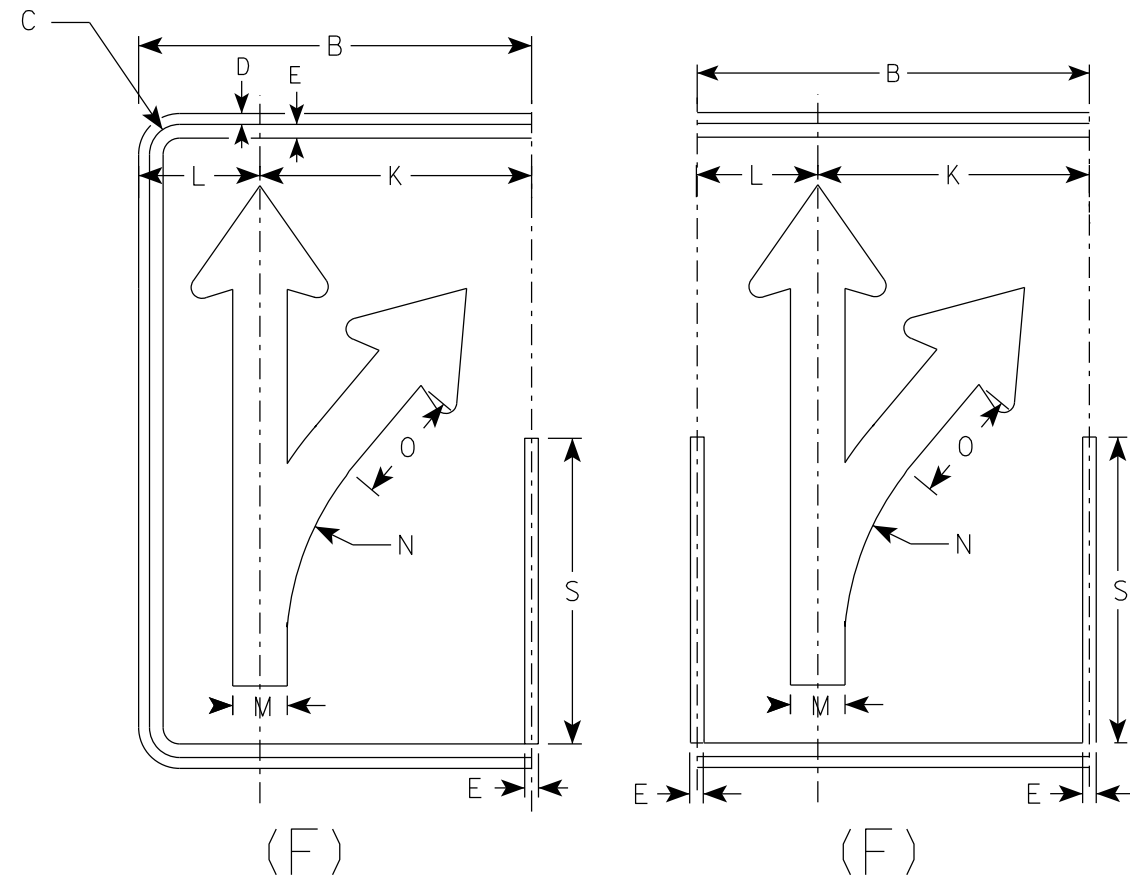
APPROVED *Matthew R Rauch*
for State Traffic Engineer

DATE 5/21/19 PLATE NO. R3-8.1

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - None



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30	18	1 3/8	1/2	5/8		18 1/4	4 3/4	3 1/4	3	12 1/2	5 1/2	2 1/2	13 1/4	5 1/8	3/8	4 3/4	2 5/8	14								3.75
2M	30	18	1 3/8	1/2	5/8		18 1/4	4 3/4	3 1/4	3	12 1/2	5 1/2	2 1/2	13 1/4	5 1/8	3/8	4 3/4	2 5/8	14								3.75
3	36	24	1 3/8	1/2	5/8		21 7/8	5 5/8	4	4 7/8	16 1/8	7 3/4	3	15 7/8	6 1/8	1/2	5 3/4	3 1/8	16 3/4								6.0
4	48	30	2 1/4	3/4	1		29 1/8	7 1/2	5 1/4	5 3/8	20 1/2	9 1/2	4	21 1/4	8 1/4	5/8	7 5/8	4 1/4	22 3/8								10.0
5	48	30	2 1/4	3/4	1		29 1/8	7 1/2	5 1/4	5 3/8	20 1/2	9 1/2	4	21 1/4	8 1/4	5/8	7 5/8	4 1/4	22 3/8								10.0

STANDARD SIGN
R3-8 (F) Arrow

WISCONSIN DEPT OF TRANSPORTATION

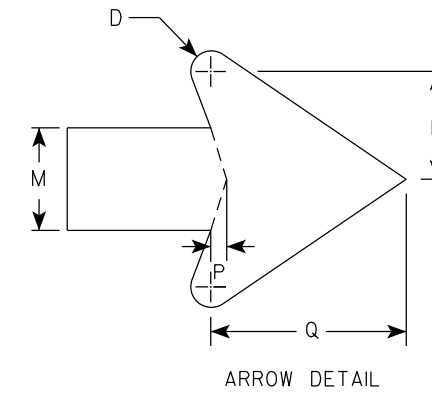
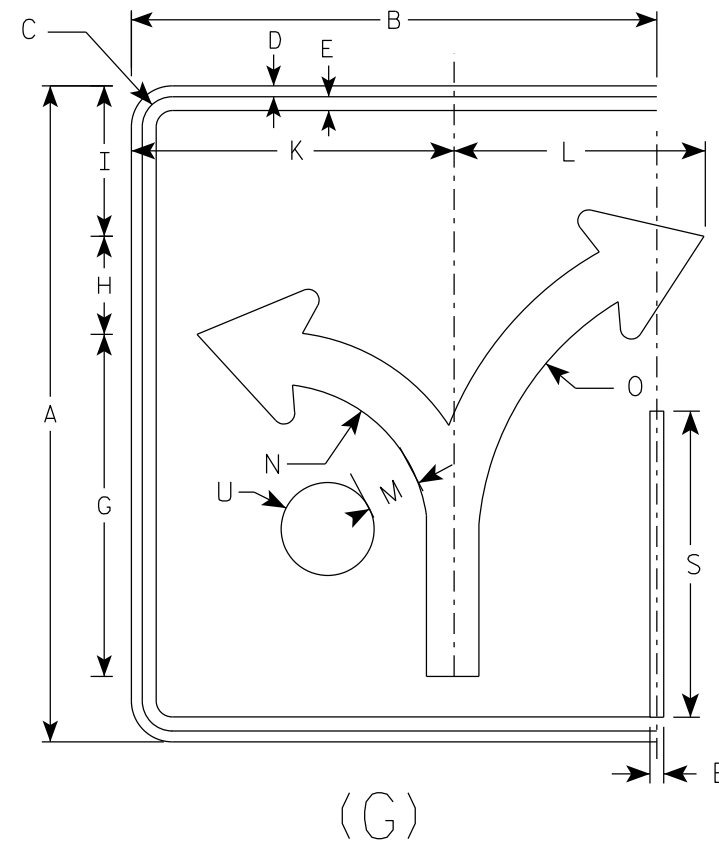
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 5/21/19 PLATE NO. R3-8.1

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - None



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30	24	1 3/8	1/2	5/8		15 5/8	4 1/2	6 7/8		14 3/4	11 1/2	2 3/8	7	13 1/4	3/8	4 1/2	2 1/2	14		2 1/8						5.0
2M	30	24	1 3/8	1/2	5/8		15 5/8	4 1/2	6 7/8		14 3/4	11 1/2	2 3/8	7	13 1/4	3/8	4 1/2	2 1/2	14		2 1/8						5.0
3	36	30	1 3/8	1/2	5/8		18 3/4	5 1/2	8 1/4		17 1/4	17 1/4	2 7/8	8 3/8	16	1/2	5 1/2	3	16 3/4		2 1/2						7.5
4	48	36	2 1/4	3/4	1		24 7/8	7 1/4	11		23 1/8	18	3 3/4	11 1/8	21 1/4	5/8	7 1/8	4	22 3/8		3 3/8						12.0
5	48	36	2 1/4	3/4	1		24 7/8	7 1/4	11		23 1/8	18	3 3/4	11 1/8	21 1/4	5/8	7 1/8	4	22 3/8		3 3/8						12.0

STANDARD SIGN
R3-8 (G) Arrow

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Rauch*
For State Traffic Engineer

DATE 5/21/19 PLATE NO. R3-8.1

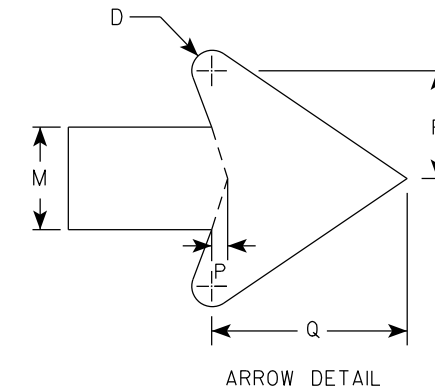
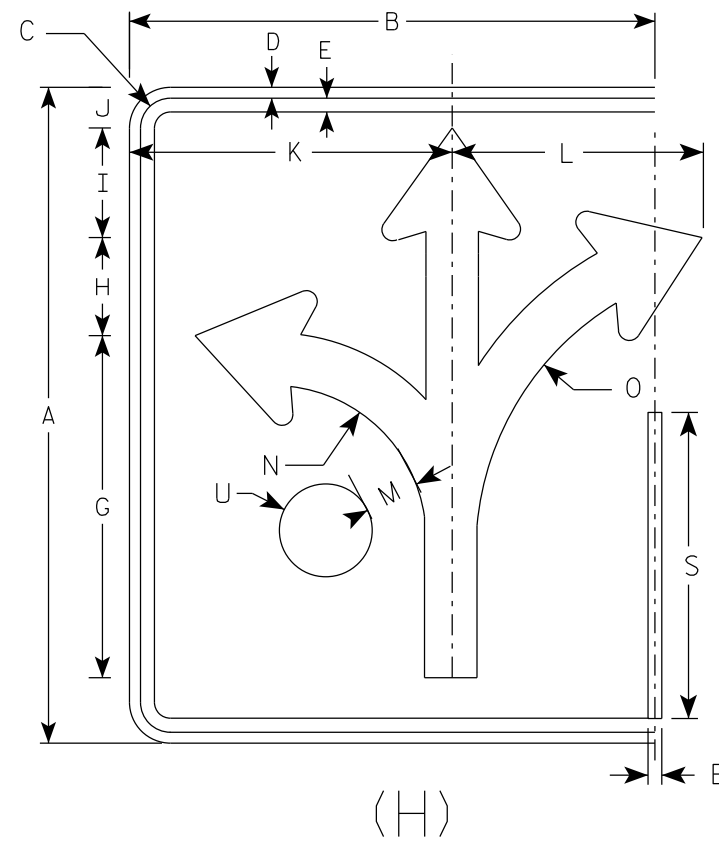
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

7

7

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - None



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30	24	1 3/8	1/2	5/8		15 5/8	4 1/2	5	1 7/8	14 3/4	11 1/2	2 3/8	7	13 1/4	3/8	4 1/2	2 1/2	14		2 1/8						5.0
2M	30	24	1 3/8	1/2	5/8		15 5/8	4 1/2	5	1 7/8	14 3/4	11 1/2	2 3/8	7	13 1/4	3/8	4 1/2	2 1/2	14		2 1/8						5.0
3	36	30	1 3/8	1/2	5/8		18 3/4	5 1/2	6	3 1/8	17 1/4	17 1/4	2 7/8	8 3/8	16	1/2	5 1/2	3	16 3/4		2 1/2						7.5
4	48	36	2 1/4	3/4	1		24 7/8	7 1/4	7 7/8	3 1/8	23 1/8	18	3 3/4	11 1/8	21 1/4	5/8	7 1/8	4	22 3/8		3 3/8						12.0
5	48	36	2 1/4	3/4	1		24 7/8	7 1/4	7 7/8	3 1/8	23 1/8	18	3 3/4	11 1/8	21 1/4	5/8	7 1/8	4	22 3/8		3 3/8						12.0

STANDARD SIGN
R3-8 (H) Arrow

WISCONSIN DEPT OF TRANSPORTATION

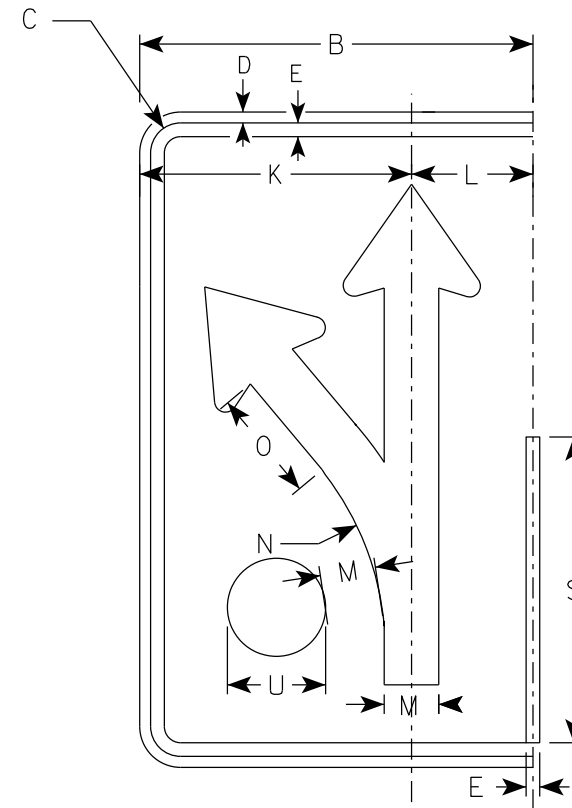
APPROVED *Matthew R Rauch*
For State Traffic Engineer

DATE 5/21/19 PLATE NO. R3-8.1

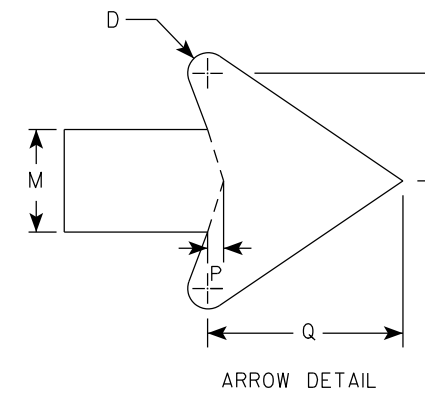
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - None



(I)



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30	18	1 3/8	1/2	5/8		18 1/4	4 3/4	3 1/4	3	12 1/2	5 1/2	2 1/2	13 1/4	5 1/8	3/8	4 3/4	2 5/8	14		2 1/8						3.75
2M	30	18	1 3/8	1/2	5/8		18 1/4	4 3/4	3 1/4	3	12 1/2	5 1/2	2 1/2	13 1/4	5 1/8	3/8	4 3/4	2 5/8	14		2 1/8						3.75
3	36	24	1 3/8	1/2	5/8		21 7/8	5 5/8	4	4 7/8	16 1/8	7 3/4	3	15 7/8	6 1/8	1/2	5 3/4	3 1/8	16 3/4		2 1/2						6.0
4	48	30	2 1/4	3/4	1		29 1/8	7 1/2	5 1/4	5 3/8	20 1/2	9 1/2	4	21 1/4	8 1/4	5/8	7 5/8	4 1/4	22 3/8		3 3/8						10.0
5	48	30	2 1/4	3/4	1		29 1/8	7 1/2	5 1/4	5 3/8	20 1/2	9 1/2	4	21 1/4	8 1/4	5/8	7 5/8	4 1/4	22 3/8		3 3/8						10.0

STANDARD SIGN
R3-8 (I) Arrow

WISCONSIN DEPT OF TRANSPORTATION

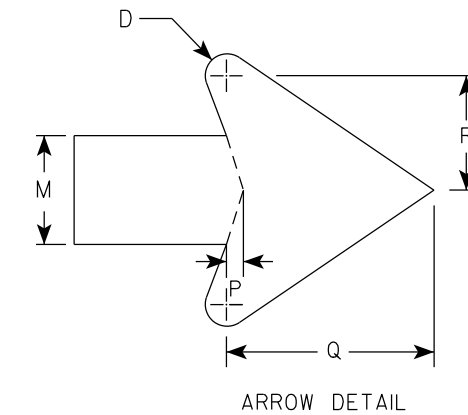
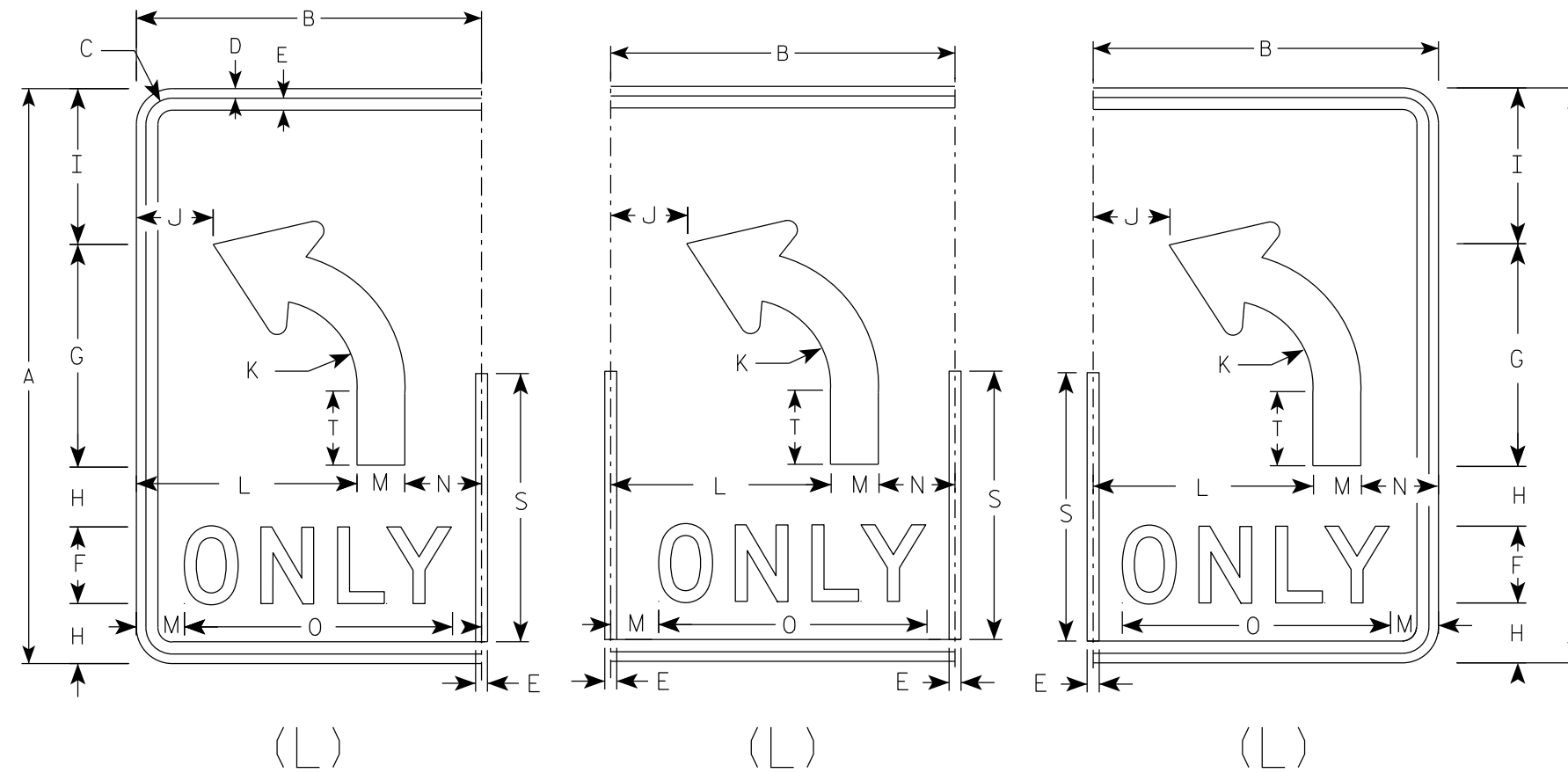
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 5/21/19 PLATE NO. R3-8.1

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - D



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30	18	1 3/8	1/2	5/8	4	11 5/8	3 1/8	8 1/8	4	4 1/2	11 1/2	2 1/2	4	14	3/8	4 3/4	2 5/8	14	3 7/8							3.75
2M	30	18	1 3/8	1/2	5/8	4	11 5/8	3 1/8	8 1/8	4	4 1/2	11 1/2	2 1/2	4	14	3/8	4 3/4	2 5/8	14	3 7/8							3.75
3	36	24	1 3/8	1/2	5/8	5	14	3 1/2	9 3/4		5 3/8	15	3	6	17 5/8	1/2	5 3/4	3 1/8	16 3/4	4 5/8							6.0
4	48	30	2 1/4	3/4	1	6	18 5/8	5 1/8	13 1/8	6 1/8	7 1/4	18	4	8	21 3/4	5/8	7 5/8	4 1/4	22 3/8	6 1/4							10.0
5	48	30	2 1/4	3/4	1	6	18 5/8	5 1/8	13 1/8	6 1/8	7 1/4	18	4	8	21 3/4	5/8	7 5/8	4 1/4	22 3/8	6 1/4							10.0

STANDARD SIGN
R3-8 (L) Arrow

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 5/21/19 PLATE NO. R3-8.1

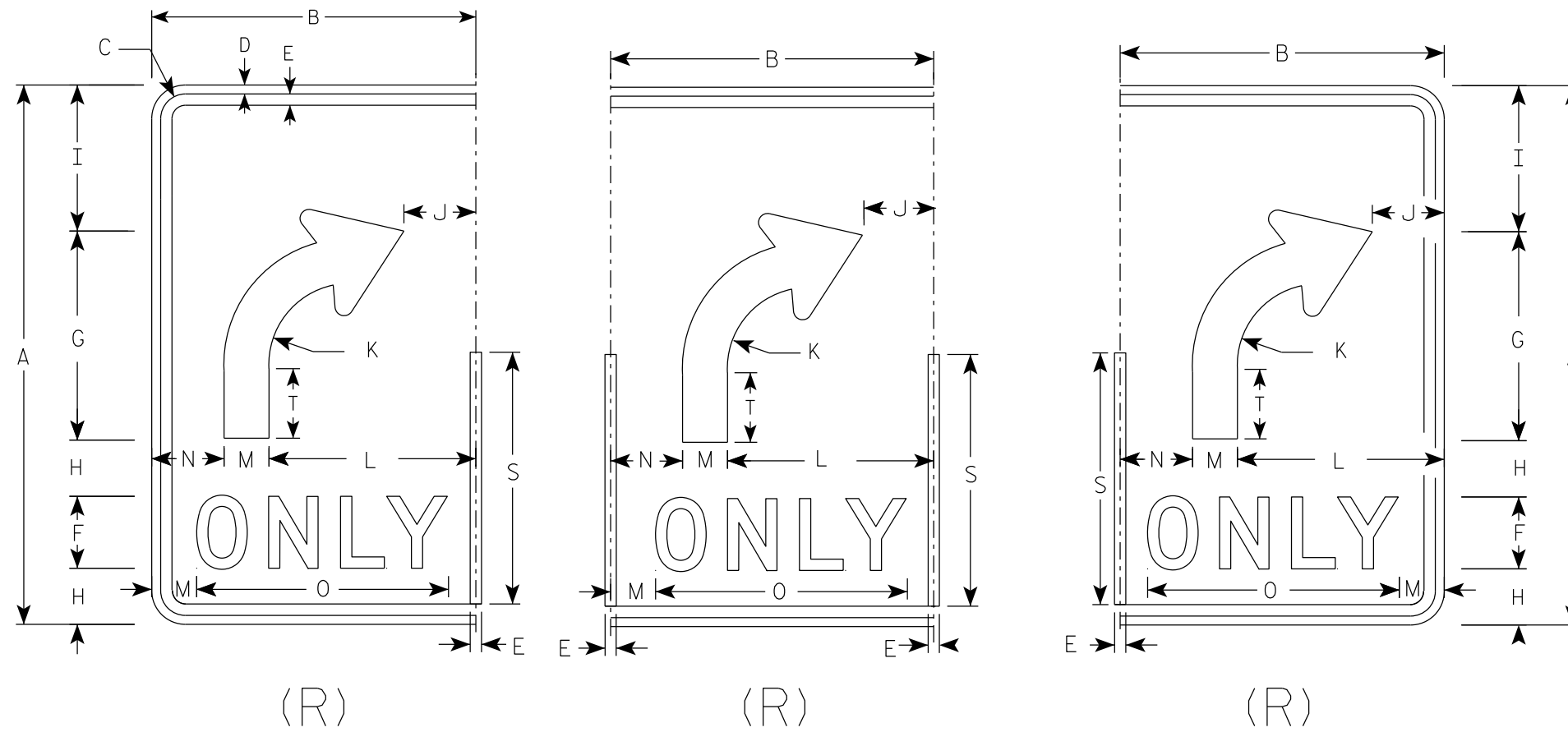
PROJECT NO:

SHEET NO:

E

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - D



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30	18	1 3/8	1/2	5/8	4	11 5/8	3 1/8	8 1/8	4	4 1/2	11 1/2	2 1/2	4	14	3/8	4 3/4	2 5/8	14	3 7/8							3.75
2M	30	18	1 3/8	1/2	5/8	4	11 5/8	3 1/8	8 1/8	4	4 1/2	11 1/2	2 1/2	4	14	3/8	4 3/4	2 5/8	14	3 7/8							3.75
3	36	24	1 3/8	1/2	5/8	5	14	3 1/2	9 3/4	6	5 3/8	15	3	6	17 5/8	1/2	5 3/4	3 1/8	16 3/4	4 5/8							6.0
4	48	30	2 1/4	3/4	1	6	18 5/8	5 1/8	13 1/8	6 1/8	7 1/4	18	4	8	21 3/4	5/8	7 5/8	4 1/4	22 3/8	6 1/4							10.0
5	48	30	2 1/4	3/4	1	6	18 5/8	5 1/8	13 1/8	6 1/8	7 1/4	18	4	8	21 3/4	5/8	7 5/8	4 1/4	22 3/8	6 1/4							10.0

STANDARD SIGN
R3-8 (R) Arrow

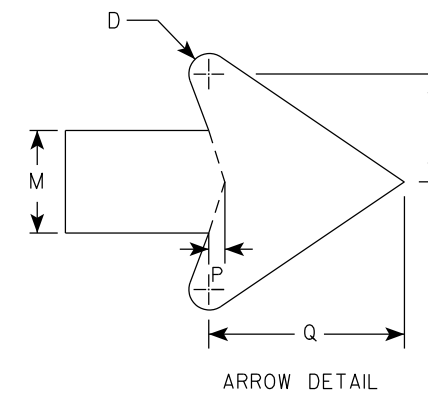
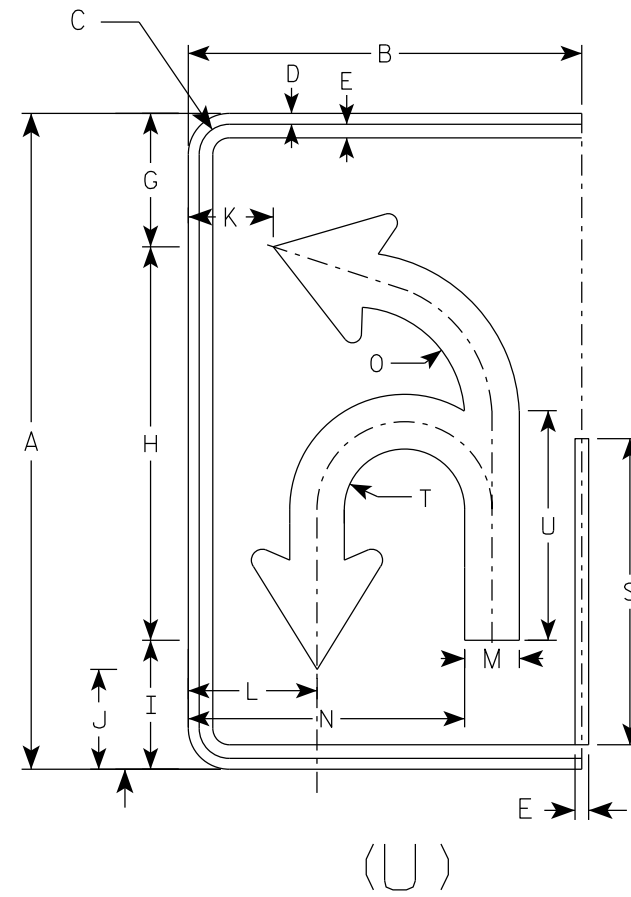
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 5/21/19 PLATE NO. R3-8.1

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - None



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30	18	1 3/8	1/2	5/8		6 1/8	18	5 7/8	4 5/8	3 7/8	5 7/8	2 1/2	12 5/8	5 1/8	3/8	4 3/4	2 5/8	14	2 3/4	10 1/2						3.75
2M	30	18	1 3/8	1/2	5/8		6 1/8	18	5 7/8	4 5/8	3 7/8	5 7/8	2 1/2	12 5/8	5 1/8	3/8	4 3/4	2 5/8	14	2 3/4	10 1/2						3.75
3	36	24	1 3/8	1/2	5/8		21 7/8	21 5/8	7 1/8	5 1/2	5 7/8	8 1/4	3	16 3/8	6 1/8	1/2	5 3/4	3 1/8	16 3/4	3 1/4	12 5/8						6.0
4	48	30	2 1/4	3/4	1		29 1/8	28 3/4	9 3/8	7 1/4	6 7/8	10	4	20 7/8	8 1/8	5/8	7 5/8	4 1/4	22 3/8	4 3/8	16 3/4						10.0
5	48	30	2 1/4	3/4	1		29 1/8	28 3/4	9 3/8	7 1/4	6 7/8	10	4	20 7/8	8 1/8	5/8	7 5/8	4 1/4	22 3/8	4 3/8	16 3/4						10.0

STANDARD SIGN
R3-8 (U) Arrow

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Rauch*
for State Traffic Engineer

DATE 5/21/19 PLATE NO. R3-8.1

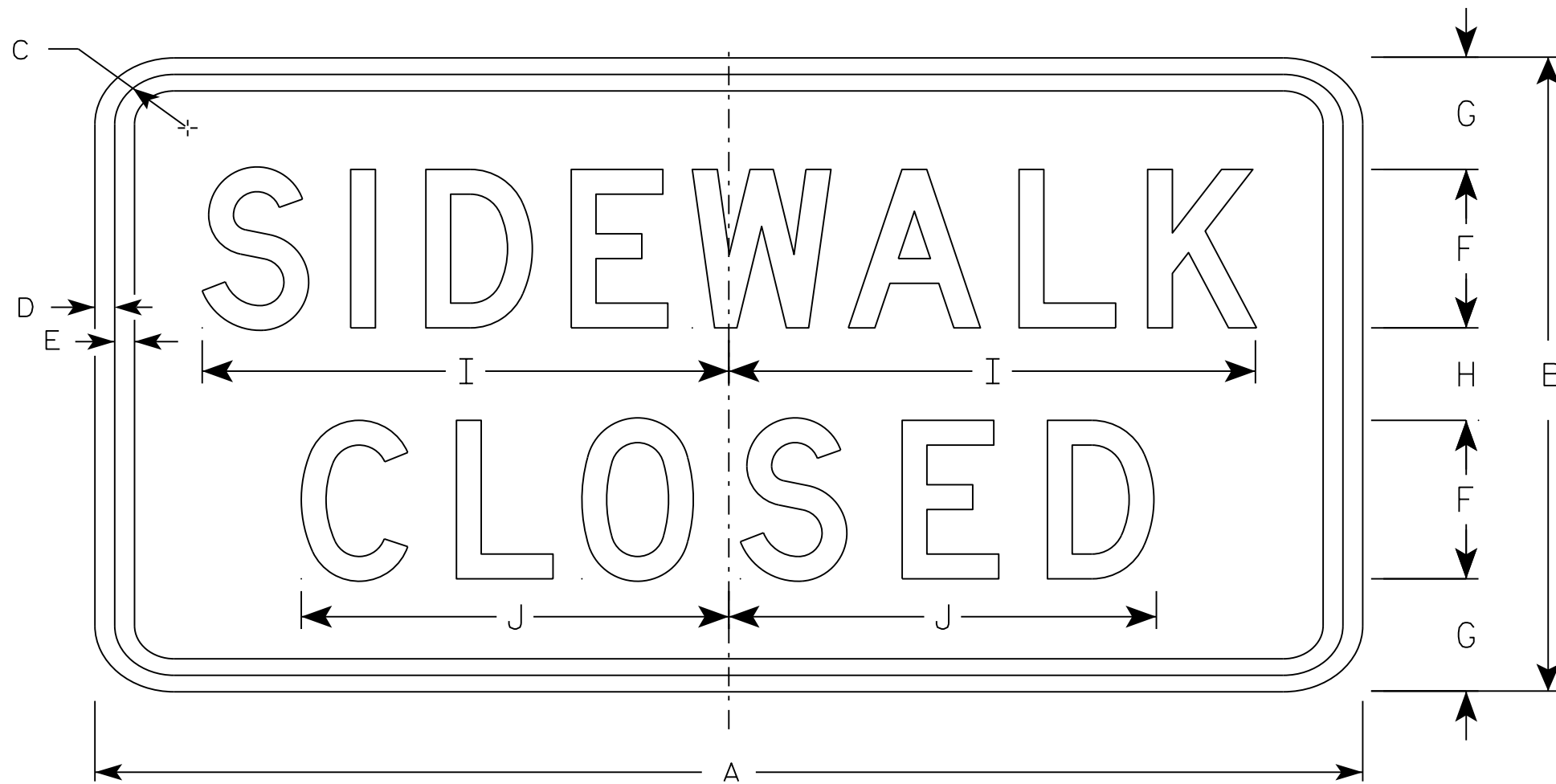
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

7

7

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Use Size 2 for Sidewalks. Use Size 3 for Paths and Trails.



R9-9

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	12	1 3/4	1/2	1/2	3	2 1/8	1 3/4	10	8 1/8																	2.0
2M	24	12	1 3/4	1/2	1/2	3	2 1/8	1 3/4	10	8 1/8																	2.0
3	30	18	1 3/4	1/2	1/2	4	3 1/2	3	12 1/2	10 1/4																	3.75
4																											
5																											

STANDARD SIGN
R9-9

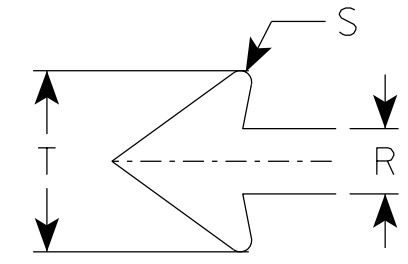
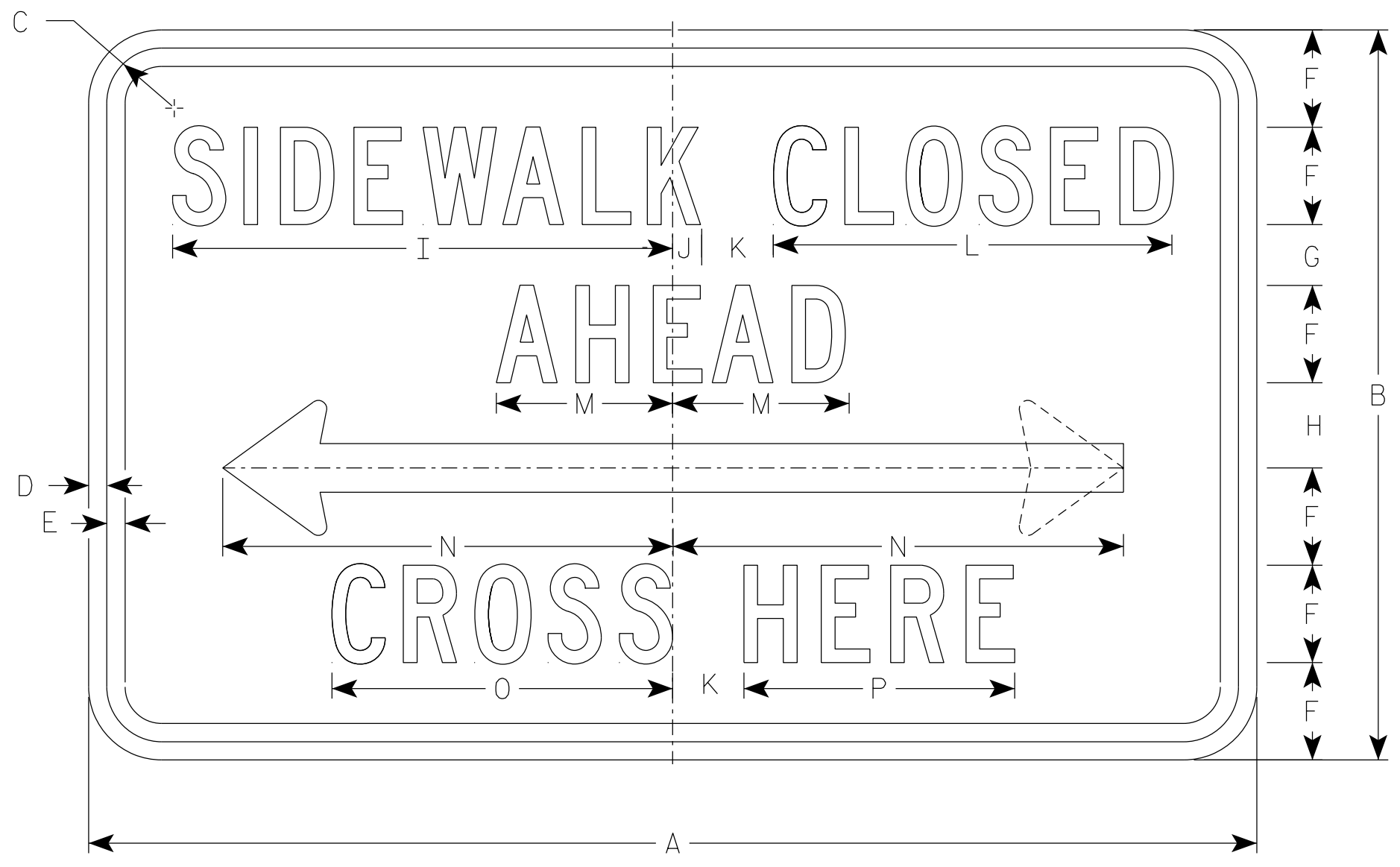
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 8/11/16 PLATE NO. R9-9.6

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - C except Size 1 is Series D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Use Size 2 for Sidewalks. Use Size 3 for Paths and Trails.
6. R9-11D (double arrow)
R9-11L (left arrow)
R9-11R (right arrow)



R9-11

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	12	1 1/8	3/8	3/8	1 1/2	1 1/2	1 1/2	9 3/4	5/8	1 1/2	7 5/8	3 1/2	9 1/4	6 5/8	5 1/8		1	1/8	2 3/4							2.0
2M	24	12	1 1/8	3/8	3/8	1 1/2	1 1/2	1 1/2	9 3/4	5/8	1 1/2	7 5/8	3 1/2	9 1/4	6 5/8	5 1/8		1	1/8	2 3/4							2.0
3	30	15	1 1/8	3/8	1/2	2	1 1/2	1 1/2	13	3/4	2	10 1/4	4 5/8	12 3/8	8 7/8	6 7/8		1 1/4	1/4	3 5/8							3.125
4																											
5																											

STANDARD SIGN
R9-11

WISCONSIN DEPT OF TRANSPORTATION

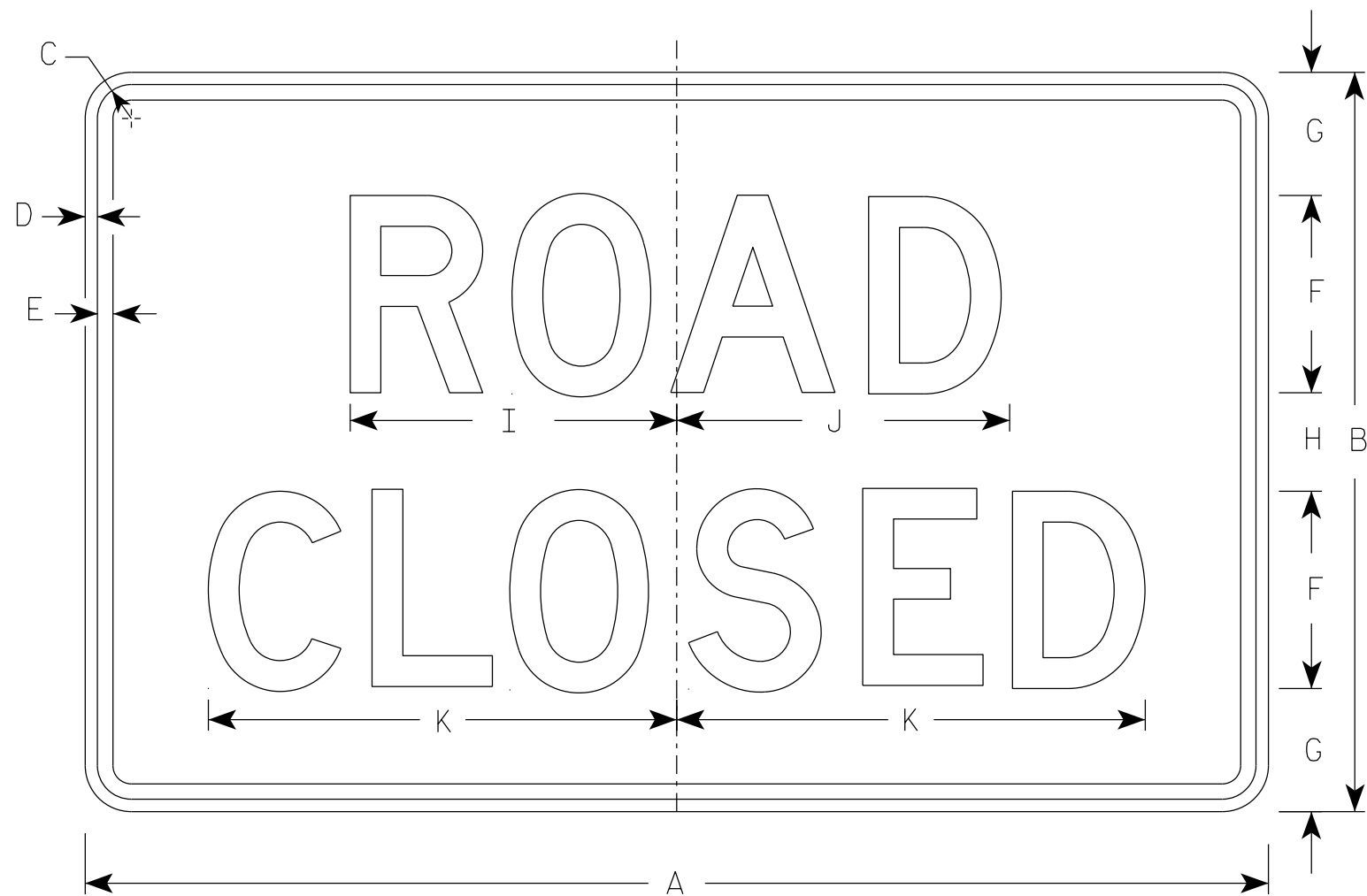
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 3/30/2021 PLATE NO. R9-11.4

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ **E**

7

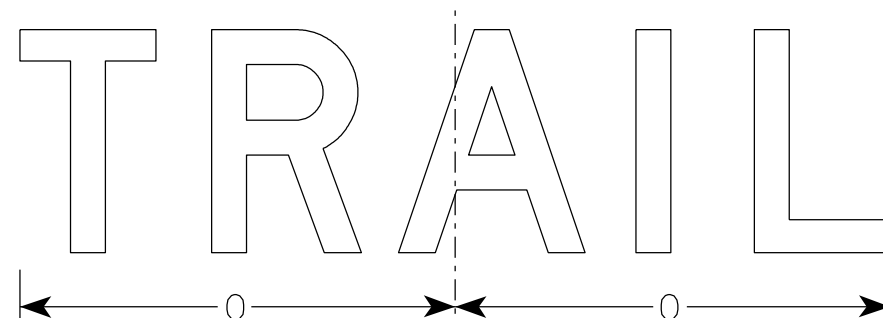
7



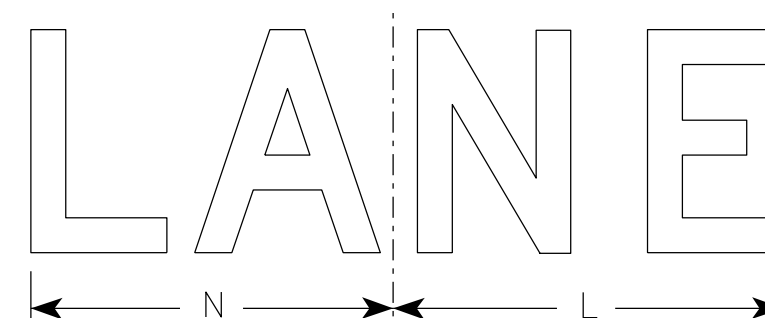
R11-2



R11-2R



R11-2T



R11-2L

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Modify the message as required.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
2M	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
3	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
4	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
5	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0

STANDARD SIGN
R11-2

WISCONSIN DEPT OF TRANSPORTATION

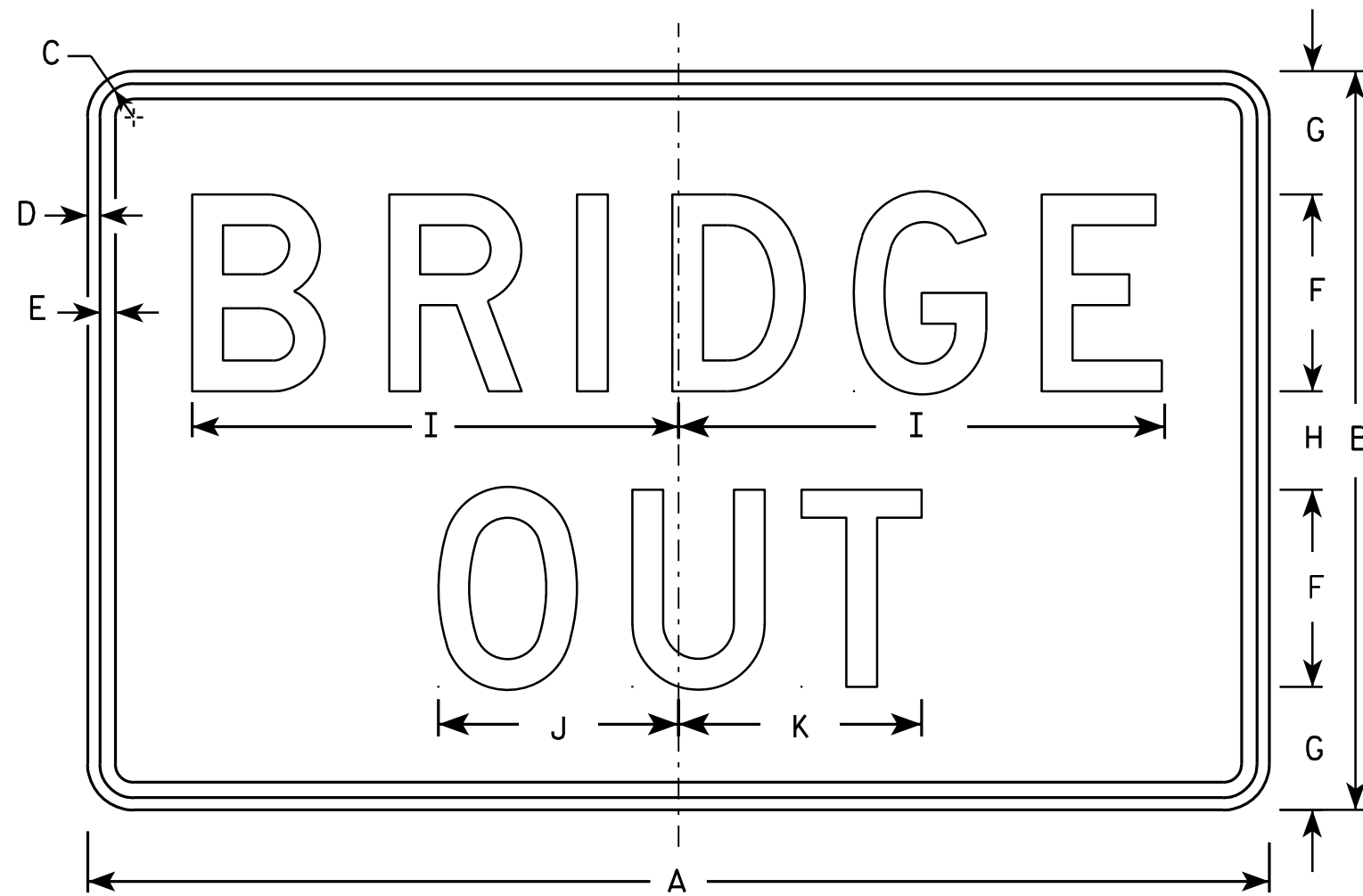
APPROVED *Matthew R Rauch*
For State Traffic Engineer

DATE 3/29/2021 PLATE NO. R11-2.11

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



R11-2B

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48	30	1 3/8	1/2	5/8	8	5	4	19 3/4	9 3/4	9 7/8																10.0
2M	48	30	1 3/8	1/2	5/8	8	5	4	19 3/4	9 3/4	9 7/8																10.0
3	48	30	1 3/8	1/2	5/8	8	5	4	19 3/4	9 3/4	9 7/8																10.0
4	48	30	1 3/8	1/2	5/8	8	5	4	19 3/4	9 3/4	9 7/8																10.0
5	48	30	1 3/8	1/2	5/8	8	5	4	19 3/4	9 3/4	9 7/8																10.0

STANDARD SIGN
R11-2B

WISCONSIN DEPT OF TRANSPORTATION

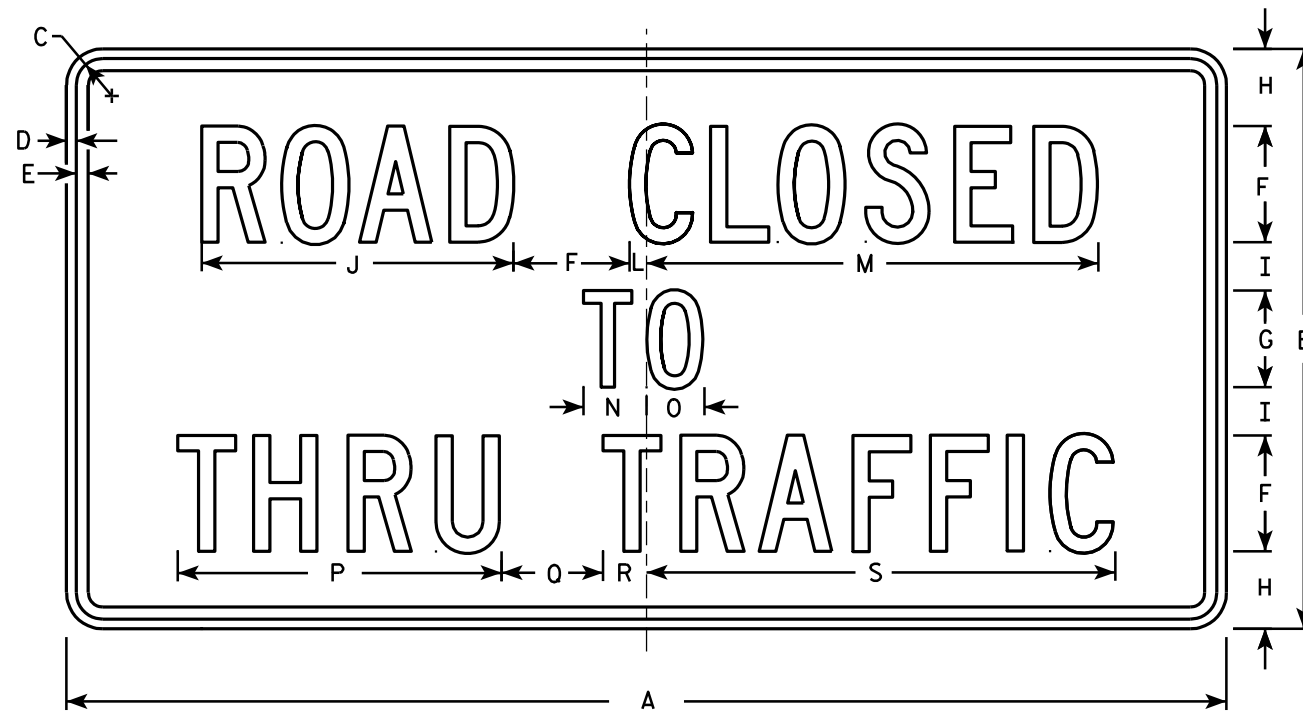
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 4/1/11 PLATE NO. R11-2B.2

PROJECT NO: _____ SHEET NO: _____ E

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



R11-4

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	60	30	1 3/8	1/2	5/8	6	5	4	2 1/2	16 1/8		7/8	23 3/8	3 1/4	3	16 3/4	5 1/4	2 1/4	24 1/4								12.5
2M	60	30	1 3/8	1/2	5/8	6	5	4	2 1/2	16 1/8		7/8	23 3/8	3 1/4	3	16 3/4	5 1/4	2 1/4	24 1/4								12.5
3																											
4																											
5																											

STANDARD SIGN
R11 - 4

WISCONSIN DEPT OF TRANSPORTATION

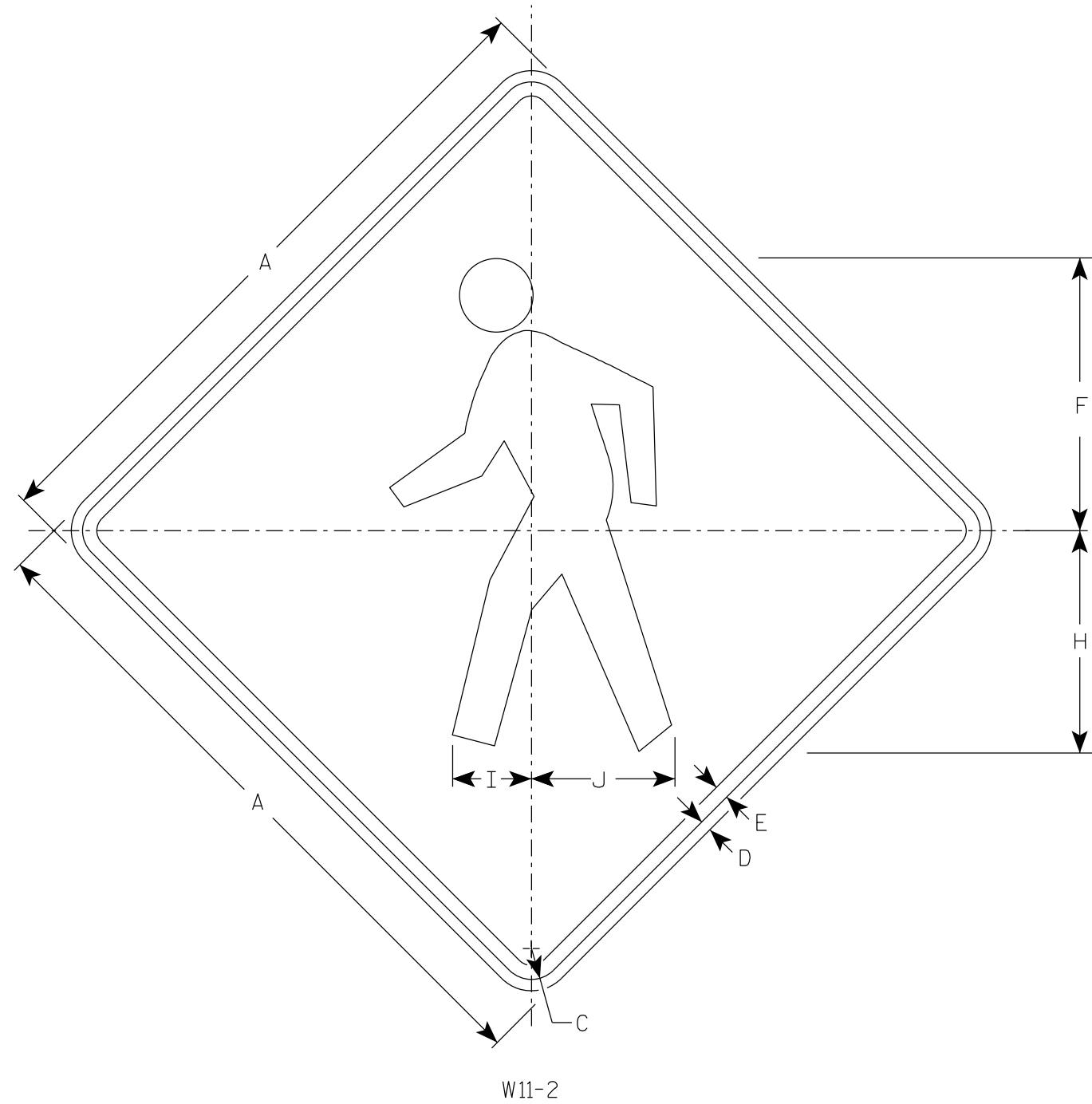
APPROVED *Matthew R. Raush*
for State Traffic Engineer

DATE 4/1/11 PLATE NO. R11-4.3

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
 Background - Yellow
 Message - Black



W11-2

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2	9 3/4		7 7/8	2 7/8	5 1/8																	4.0
2S	30		1 3/8	1/2	5/8	12 1/8		9 7/8	3 1/2	6 3/8																	6.25
2M	36		1 5/8	5/8	3/4	14 1/2		11 7/8	4 1/4	7 5/8																	9.0
3	36		1 5/8	5/8	3/4	14 1/2		11 7/8	4 1/4	7 5/8																	9.0
4	48		2 1/4	3/4	1	19 3/8		15 3/4	5 5/8	10 1/4																	16.0
5																											

STANDARD SIGN
W11-2

WISCONSIN DEPT OF TRANSPORTATION

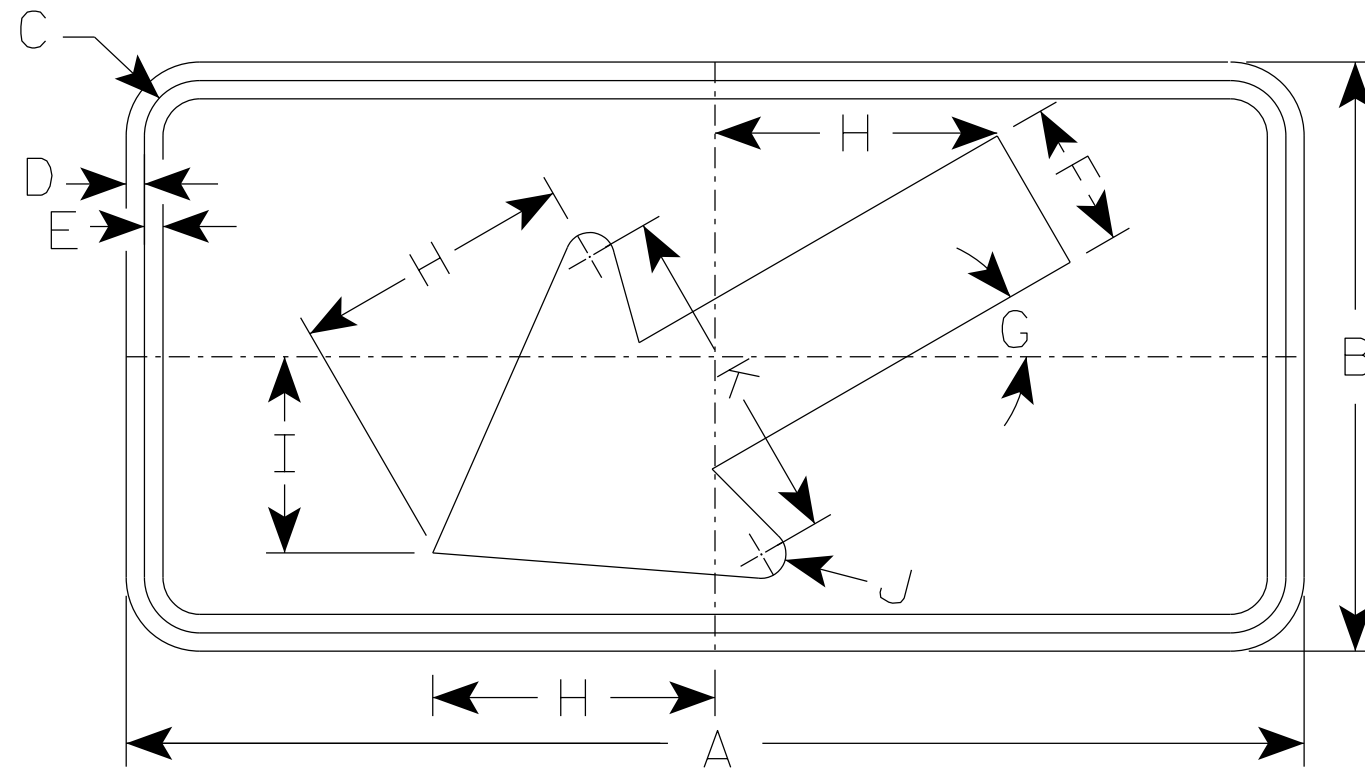
APPROVED *Matthew R Rauch*
for State Traffic Engineer

DATE 4/8/2020 PLATE NO. W11-2.8

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ **E**

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Yellow
Message - Black
3. W16-7R is the same as W16-7L
except the arrow is reversed along
the vertical centerline.



W16-7L

- * For 36" x 36" Warning Signs, use 30" x 18" W16-7L signs.
- * For 48" x 48" Warning Signs, use 48" x 24" W16-7L signs.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	12	1 1/8	3/8	3/8	3	30°	5 3/4	4	1/2	7																2.0
* 2M	30	18	1 1/8	3/8	1/2	4 1/2	30°	8 1/2	6	5/8	10 1/4																3.75
* 3	30	18	1 1/8	3/8	1/2	4 1/2	30°	8 1/2	6	5/8	10 1/4																3.75
* 4	48	24	1 3/8	1/2	5/8	6	30°	11 1/2	8	1	14																8.0
5																											

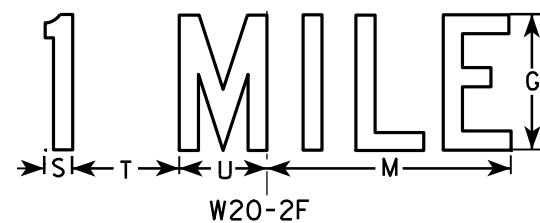
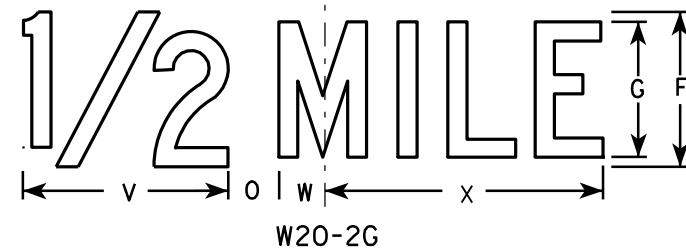
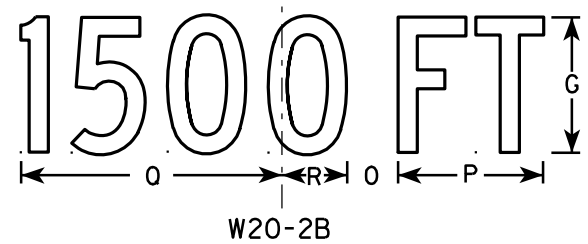
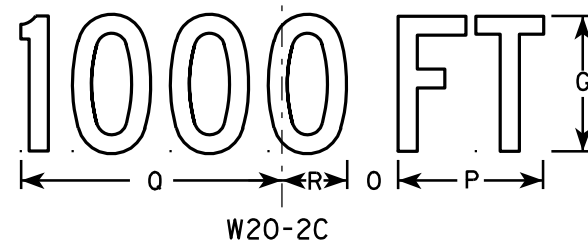
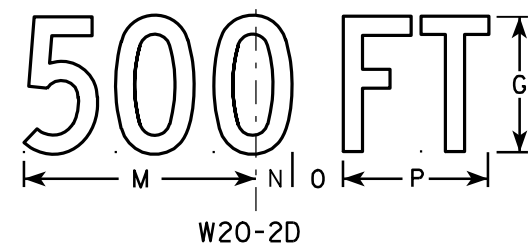
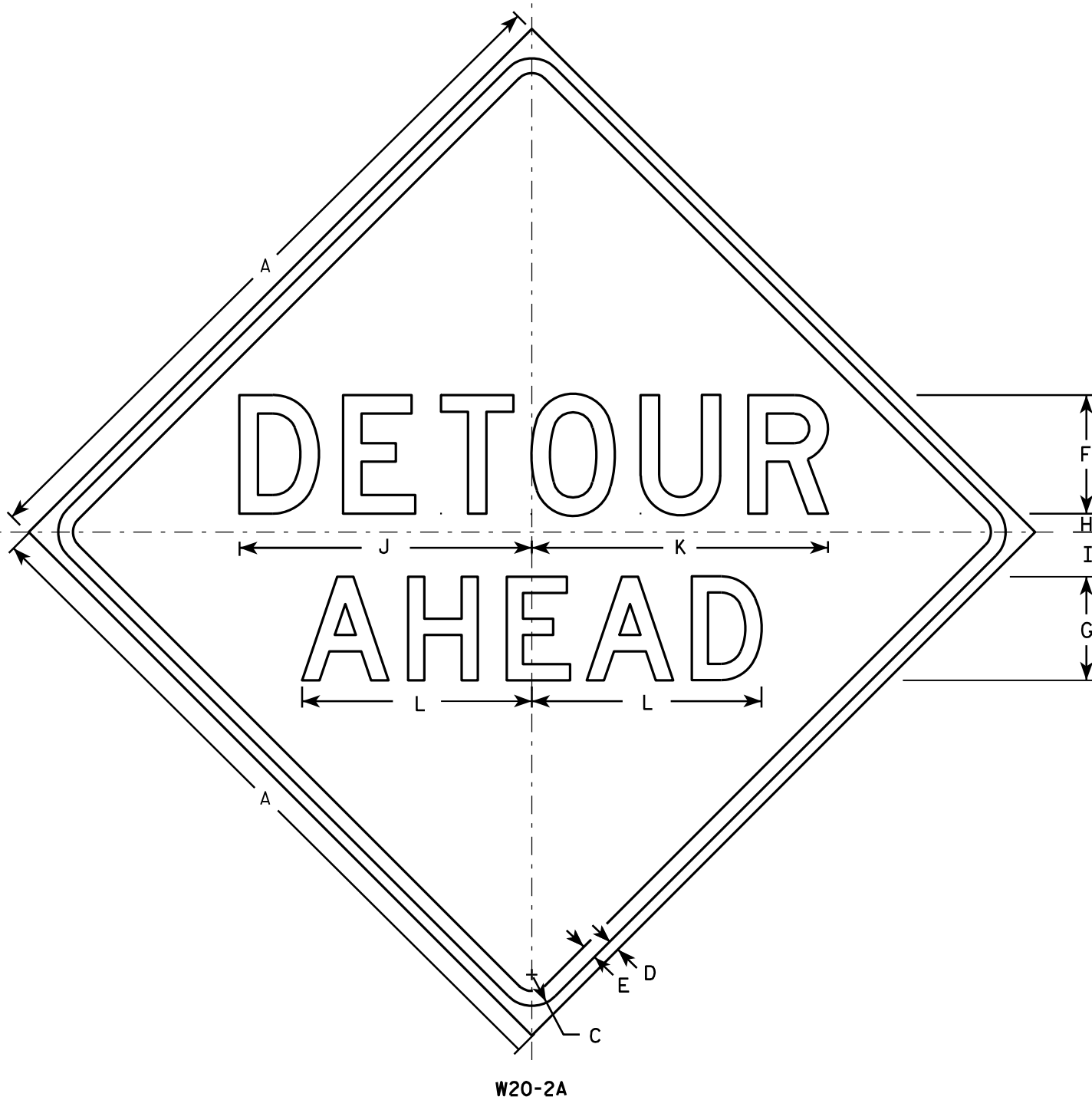
STANDARD SIGN
W16-7

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 3/16/2021 PLATE NO. W16-7.8

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - See note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Line 1 is Series D.
Line 2 is Series D for AHEAD and Series C for all other distances.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	6	5	1	2 1/4	14 3/4	15	11 5/8	9	1 3/8	1 7/8	5 5/8	10 1/8	2 1/2	1 1/8	4 1/2	3 1/2	8	1 3/4	10 3/4			9.0
2S	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
2M	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
3	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
4	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
5	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0

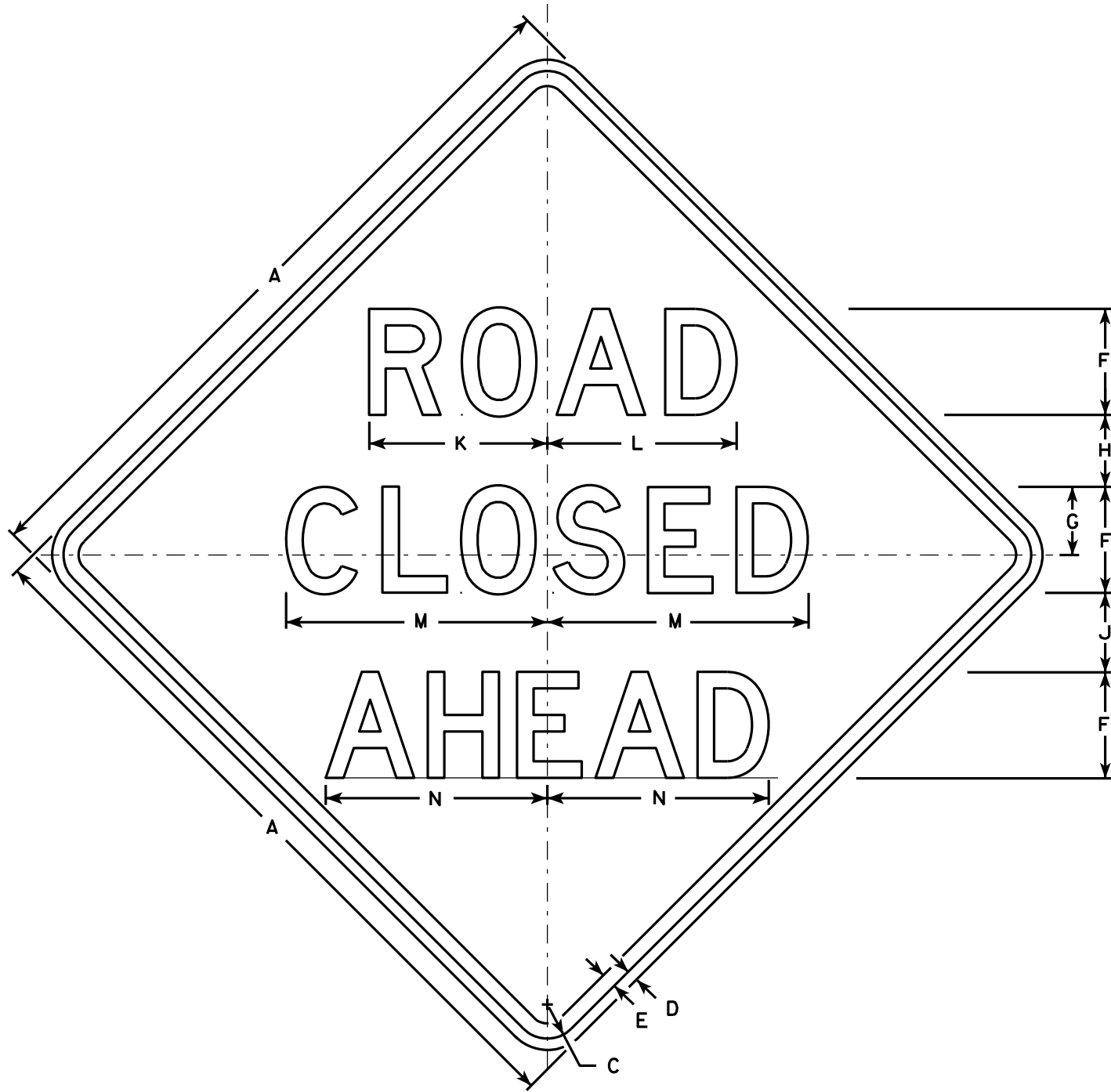
STANDARD SIGN
W20-2A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

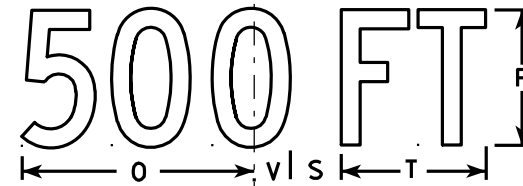
APPROVED *Matthew R. Raub*
for State Traffic Engineer

DATE 3/18/11 PLATE NO. W20-2.6

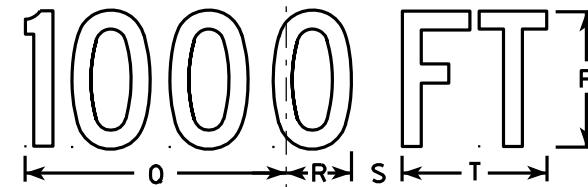
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E



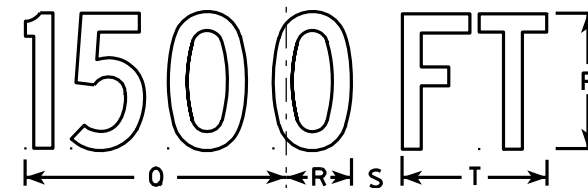
W20-3A



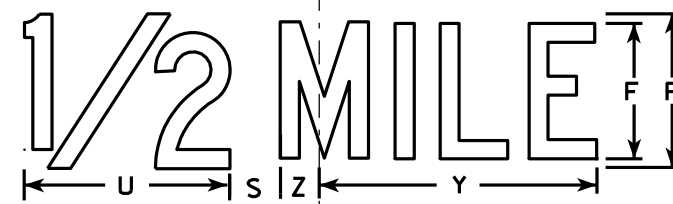
W20-3D



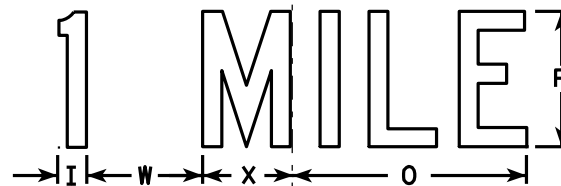
W20-3C



W20-3B



W20-3G



W20-3F

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - see note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Lines 1 and 2 are Series D.
Line 3 is Series D for AHEAD and Series C for all other distances.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	5	3 3/8	3 1/2	1 1/8	4	8 3/8	8 7/8	12 1/2	11	9	6	10 1/8	2 1/2	1 7/8	5 5/8	8	1 3/8	4 1/2	3 1/2	10 3/4	1 3/4	9.0
2S	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
2M	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
3	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
4	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
5	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0

STANDARD SIGN
W20-3A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 3/18/11 PLATE NO. W20-3.7

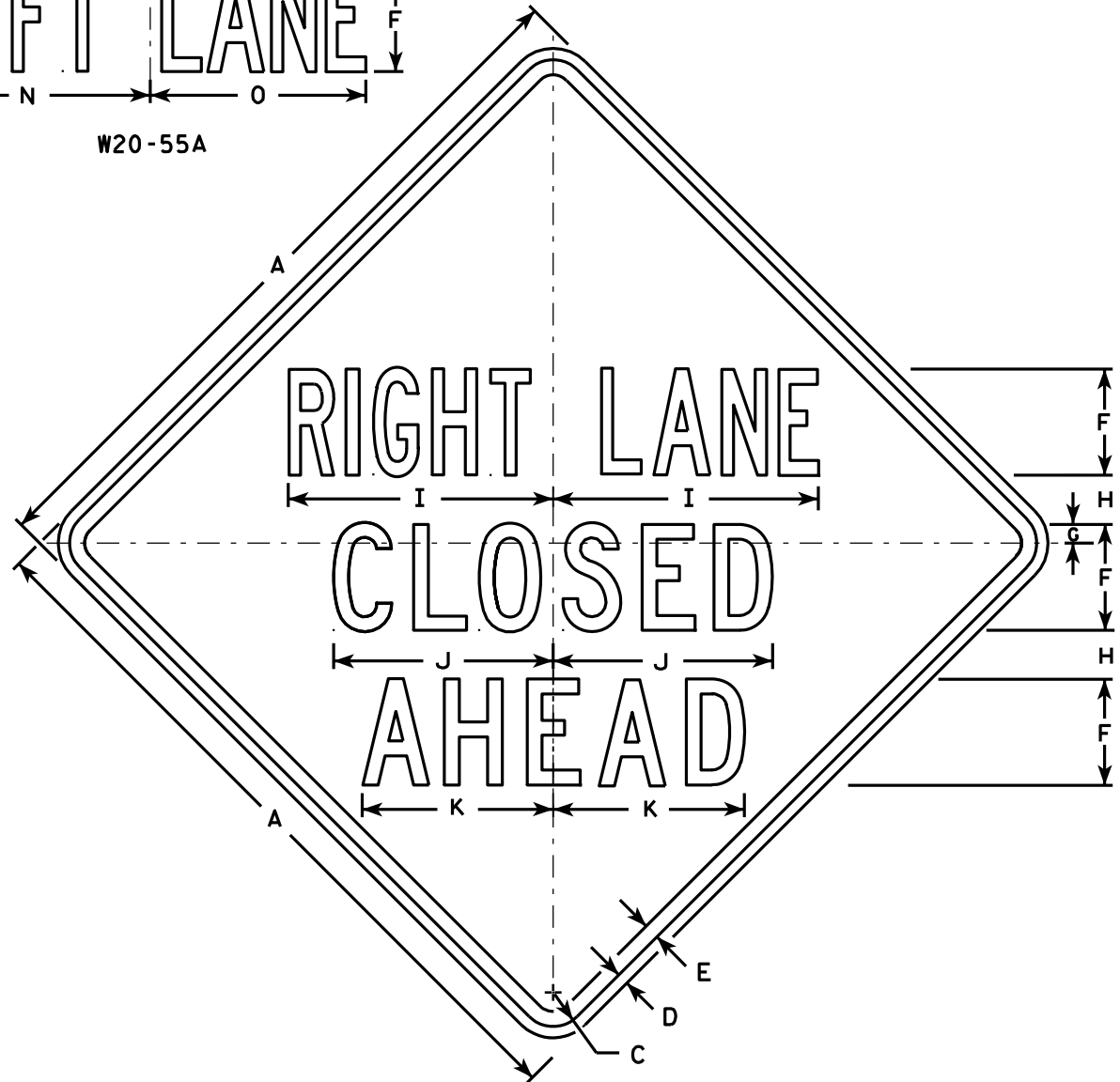
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E

CENTER LANE

W20-56A

LEFT LANE

W20-55A



W20-5A

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - See Note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. "-----LANE" is Series B.
All other copy is Series C.

500 FT

W20-5D

1000 FT

W20-5C

1500 FT

W20-5B

1/2 MILE

W20-5G

1 MILE

W20-5F

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36	6	1 5/8	5/8	3/4	5	7/8	2 1/2	13 1/8	10 3/4	9 1/2	14 1/4	13 5/8	12	12	1 3/8	1 1/8	4 1/2	3 1/2	9	1 7/8	5 5/8	10 1/8	2 1/2	1 3/4	8	9.0
2S	48	8	2 1/4	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 5/8	19	18 3/8	16	14 1/4	1 7/8	1 1/2	6	4 5/8	12	2 5/8	7 1/2	13 1/2	3 3/8	2 3/8	10 5/8	16.0
2M	48	8	2 1/4	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 5/8	19	18 3/8	16	14 1/4	1 7/8	1 1/2	6	4 5/8	12	2 5/8	7 1/2	13 1/2	3 3/8	2 3/8	10 5/8	16.0
3	48	8	2 1/4	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 5/8	19	18 3/8	16	14 1/4	1 7/8	1 1/2	6	4 5/8	12	2 5/8	7 1/2	13 1/2	3 3/8	2 3/8	10 5/8	16.0
4	48	8	2 1/4	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 5/8	19	18 3/8	16	14 1/4	1 7/8	1 1/2	6	4 5/8	12	2 5/8	7 1/2	13 1/2	3 3/8	2 3/8	10 5/8	16.0
5	48	8	2 1/4	3/4	1	7	1 1/4	3 1/4	17 1/2	14 3/8	12 5/8	19	18 3/8	16	14 1/4	1 7/8	1 1/2	6	4 5/8	12	2 5/8	7 1/2	13 1/2	3 3/8	2 3/8	10 5/8	16.0

STANDARD SIGN
W20-5A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 3/18/11 PLATE NO. W20-5.11

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

DESIGN DATA

DESIGN LOADING _____ HL-93
 INVENTORY RATING FACTOR _____ 1.263
 OPERATING RATING FACTOR _____ 1.637
 WISCONSIN STANDARD PERMIT VEHICLE (WIS-SPV) _____ 250 KIPS
 STRUCTURE IS DESIGNED FOR A FUTURE WEARING SURFACE OF 20 PSF

MATERIAL PROPERTIES

CONCRETE MASONRY, SUPERSTRUCTURE, APPROACH SLAB, & APPROACH SLAB FOOTING _____ f_c = 4,000 psi
 ALL OTHER _____ f_c = 3,500 psi
 HIGH STRENGTH BAR STEEL REINFORCEMENT _____ f_y = 60,000 psi
 STAINLESS STEEL BAR STEEL REINFORCEMENT _____ f_y = 60,000 psi

TRAFFIC DATA

ADT (2023) = 4,400
 ADT (2043) = 4,900
 DESIGN SPEED = 30 MPH

FOUNDATION DATA

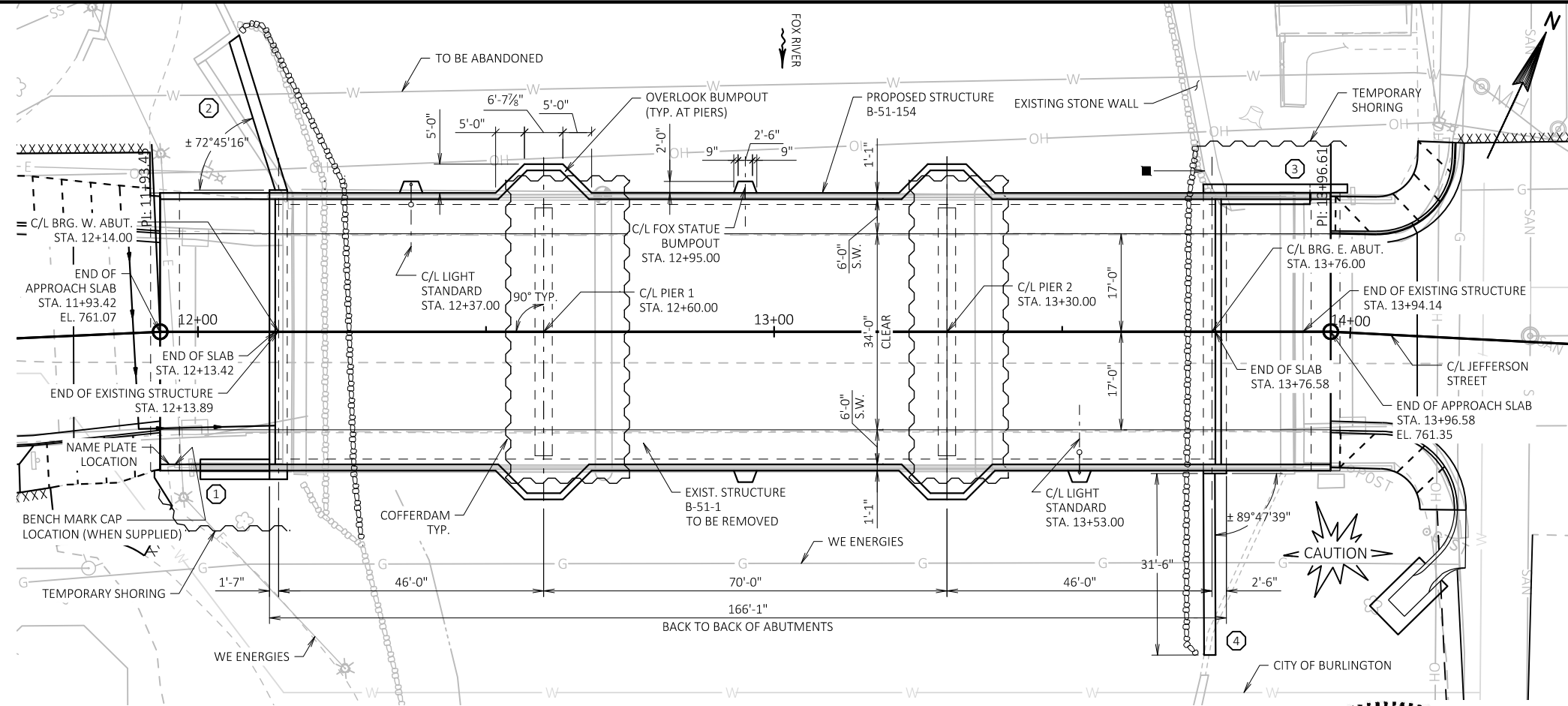
ABUTMENTS & PIERS SUPPORTED ON PILING STEEL HP 12-INCH x 53 LB SEATED IN PREBORED HOLES CORED A MINIMUM OF 3 FEET INTO BEDROCK. ESTIMATED 30' LONG AT THE WEST ABUTMENT, 30' AT THE EAST ABUTMENT AND 30' AT THE PIERS.

PILES PLACED IN PREBORED HOLES BORED INTO ROCK DO NOT REQUIRE DRIVING. PILING SHALL BE FIRMLY SEATED AFTER PLACEMENT. THE FACTORED AXIAL RESISTANCE OF PILES IN COMPRESSION USED FOR DESIGN IS 220 TONS MULTIPLIED BY A RESISTANCE FACTOR OF 0.5.

ALLOWABLE SOIL BEARING PRESSURE (APPROACH SLAB FOOTING) _____ 2,000 PSF

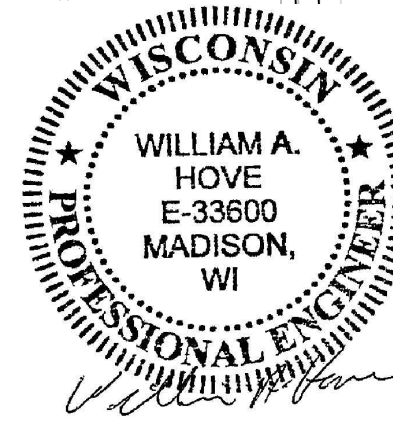
HYDRAULIC DATA

100 YEAR FREQUENCY _____
 Q₁₀₀ _____ 8,300 cfs
 VELOCITY _____ 5.81 fps
 HIGH WATER _____ EL. 758.83
 WATERWAY AREA _____ 1428 ft²
 DRAINAGE AREA _____ 767 mi²
 SCOUR CRITICAL CODE _____ 5
 OVERTOPPING FREQUENCY _____ N/A
 2 YEAR FREQUENCY _____
 Q₂ _____ 2615 cfs
 HIGH WATER _____ EL. 754.03
 Q₂ VELOCITY _____ 3.28 fps



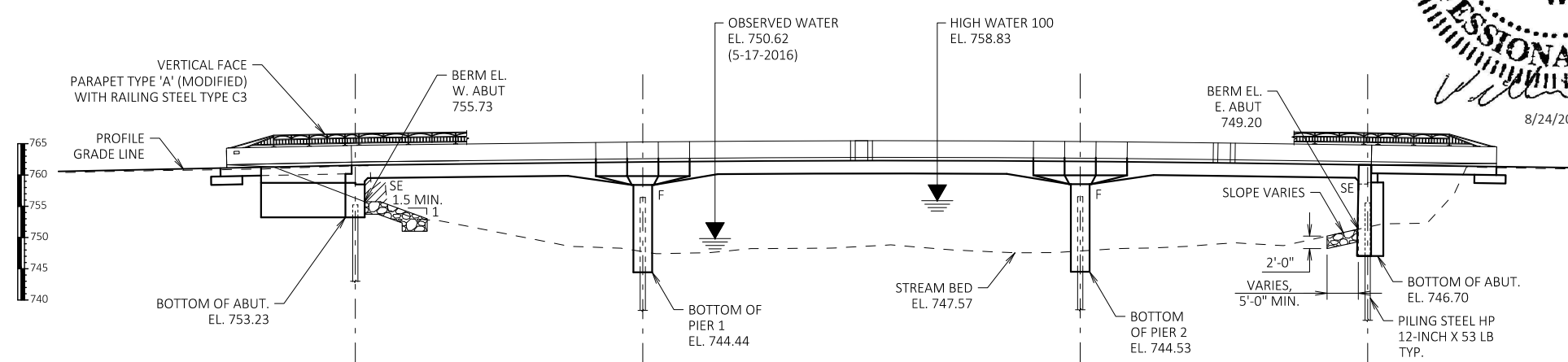
PLAN

(THREE SPAN CONCRETE HAUNCHED SLAB BRIDGE)



LIST OF DRAWINGS

1. GENERAL PLAN
2. TYPICAL SECTION, NOTES & QUANTITIES
3. MISCELLANEOUS DETAILS
4. SUBSURFACE EXPLORATION
5. WEST ABUTMENT - 1
6. WEST ABUTMENT - 2
7. WEST ABUTMENT - 3
8. EAST ABUTMENT - 1
9. EAST ABUTMENT - 2
10. EAST ABUTMENT - 3
11. EAST ABUTMENT - 4
12. PIER 1
13. PIER 2
14. SUPERSTRUCTURE PLAN
15. SUPERSTRUCTURE SECTIONS
16. SUPERSTRUCTURE & SIDEWALK SECTIONS
17. SUPERSTRUCTURE BUMPOUT DETAILS
18. SUPERSTRUCTURE DETAILS
19. LIGHT STANDARD AND JUNCTION BOX DETAILS
20. SUPERSTRUCTURE REINFORCEMENT
21. CONDUIT DETAILS
22. PARAPET & RAILING - 1
23. PARAPET & RAILING - 2
24. SE WING RAILING
25. NW WING RAILING
26. STRUCTURAL APPROACH SLAB
27. STRUCTURAL APPROACH SLAB DETAILS
28. AESTHETIC DETAILS

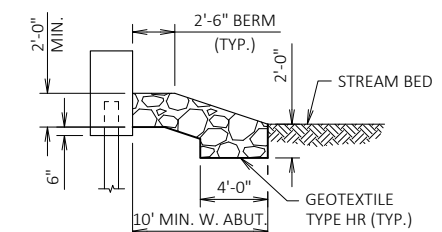


ELEVATION

(LOOKING NORTH)

LEGEND

- INDICATES WING NUMBER
- ▨ AREA TO EXCAVATE INCLUDED IN "EXCAVATION FOR STRUCTURES BRIDGES"
- SALVAGE AND REBUILD EXISTING STONE RETAINING WALL AS REQUIRED AND AS DIRECTED BY THE ENGINEER. COST INCLUDED IN "EXCAVATION FOR STRUCTURES".

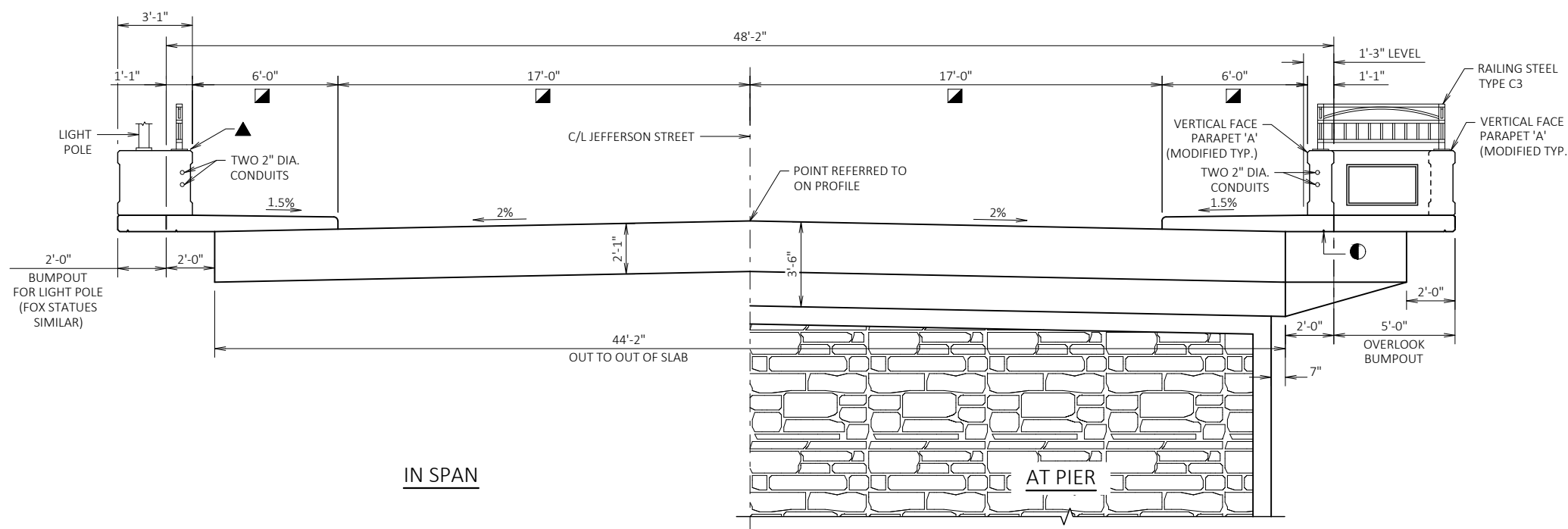


W. ABUT. RIPRAP DETAIL

BENCH MARKS

NO.	STATION	NORTHING	EASTING	DESCRIPTION	ELEV.
CP5	13+76.88, 72.17' RT.	168949.480	508638.915	PK NAIL	752.74
CP6	11+89.46, 41.65' RT.	168897.986	508456.142	PK NAIL	760.98

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION ACCEPTED <i>[Signature]</i> SDR 08/25/22 CHIEF STRUCTURES DESIGN ENGINEER DATE			
STRUCTURE B-51-154 JEFFERSON STREET OVER FOX RIVER			
COUNTY RACINE		TOWN/CITY/VILLAGE BURLINGTON	
DESIGN SPEC. AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS			
DESIGNED BY BH	DESIGN CK'D. ETP	DRAWN BY PKF	PLANS CK'D. BH
GENERAL PLAN			SHEET 1 OF 28



CROSS SECTION THRU BRIDGE
(LOOKING EAST)

GENERAL NOTES

- DRAWINGS SHALL NOT BE SCALED.
- BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2" CLEAR UNLESS SHOWN OR NOTED OTHERWISE.
- THE FIRST DIGIT OF A THREE DIGIT BAR NO. AND THE FIRST TWO DIGITS OF A FOUR DIGIT BAR NO. SIGNIFIES THE BAR SIZE.
- JOINT FILLER SHALL CONFORM TO THE REQUIREMENTS OF AASHTO DESIGNATION M153 TYPE I, II OR III OR AASHTO DESIGNATION M213.
- THE SLOPE OF THE FILL IN FRONT OF THE ABUTMENTS SHALL BE COVERED WITH RIPRAP HEAVY AND GEOTEXTILE TYPE HR TO THE EXTENT SHOWN ON THE GENERAL PLAN SHEET AND IN THE ABUTMENT DETAILS, OR AS DIRECTED BY THE ENGINEER.
- SLAB FALSEWORK SHALL BE SUPPORTED ON PILES OR THE SUBSTRUCTURE UNLESS AN ALTERNATE METHOD IS APPROVED BY THE ENGINEER.
- THE EXISTING GROUND LINE OR THE STREAM BED SHALL BE THE UPPER LIMIT FOR EXCAVATION FOR STRUCTURES.
- THE EXISTING STRUCTURE B-51-1, TO BE REMOVED, IS A THREE SPAN STEEL DECK GIRDER BRIDGE, 180.25 FT. LONG WITH A 34.0 FT. CLEAR ROADWAY WIDTH.
- AT THE BACKFACE OF ABUTMENT ALL VOLUME WHICH CANNOT BE PLACED BEFORE ABUTMENT CONSTRUCTION AND IS NOT OCCUPIED BY THE NEW STRUCTURE SHALL BE BACKFILLED WITH BACKFILL STRUCTURE. SEE MISCELLANEOUS DETAILS SHEET FOR ADDITIONAL INFORMATION.
- CONCRETE POURED UNDER WATER WILL BE ALLOWED AND SHALL BE DONE IN ACCORDANCE WITH SECTION 502.3.5.3 OF THE STANDARD SPECIFICATIONS.
- CONTRACTOR TO REMOVE AND SALVAGE STREAM GAUGE STATION EQUIPMENT FOR THE CITY TO PICK-UP. WORK IS COVERED UNDER THE "REMOVING STRUCTURE OVER WATERWAY" BID ITEM.

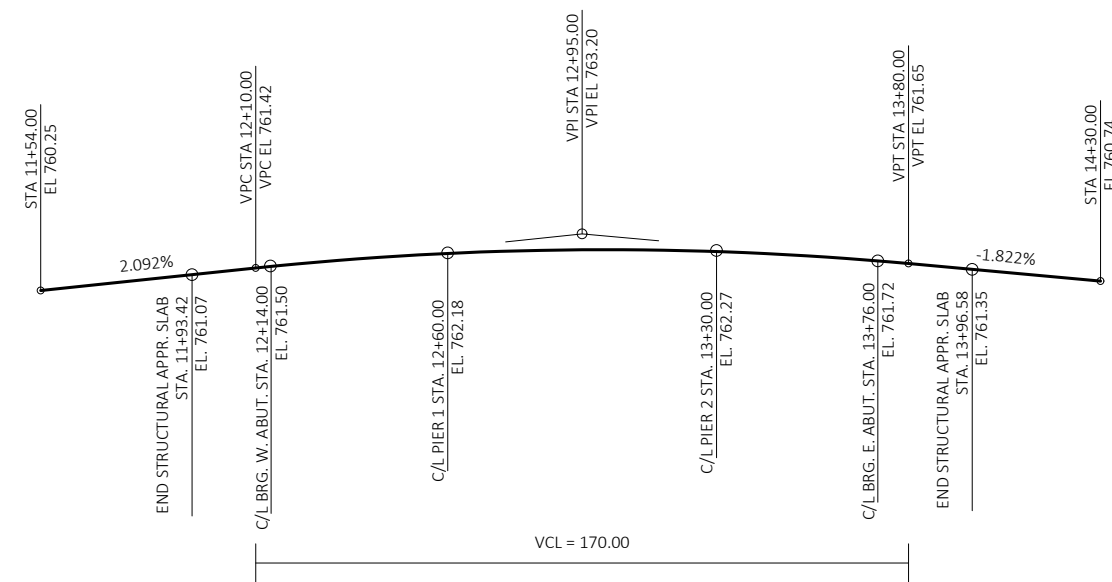
LEGEND

- 3/4" V-GROOVE. TERMINATE 6" FROM FRONT FACE OF ABUTMENTS.
- COAT WITH "PROTECTIVE SURFACE TREATMENT" PER THE STANDARD SPECIFICATIONS.
- ▲ COAT TOP OF PARAPETS WITH "PIGMENTED SURFACE SEALER" PER THE STANDARD SPECIFICATIONS. FRONT AND BACK OF PARAPETS TO BE STAINED.
- QUANTITY INCLUDES PREBORING THROUGH EXISTING FOUNDATION, THAT IS NOT IN CONFLICT WITH THE ABUTMENT LIMITS, BUT WHEN IN CONFLICT WITH PLACEMENT OF PROPOSED PILING. ALTERNATIVELY, CONTRACTOR MAY COMPLETELY REMOVE FOUNDATION. COST OF REMOVAL IS INCLUDED IN "REMOVING STRUCTURE" ITEM.
- ITEM INCLUDES PARTIAL REMOVAL OF EXISTING RETAINING WALLS AS REQUIRED TO CONSTRUCT NEW STRUCTURE.

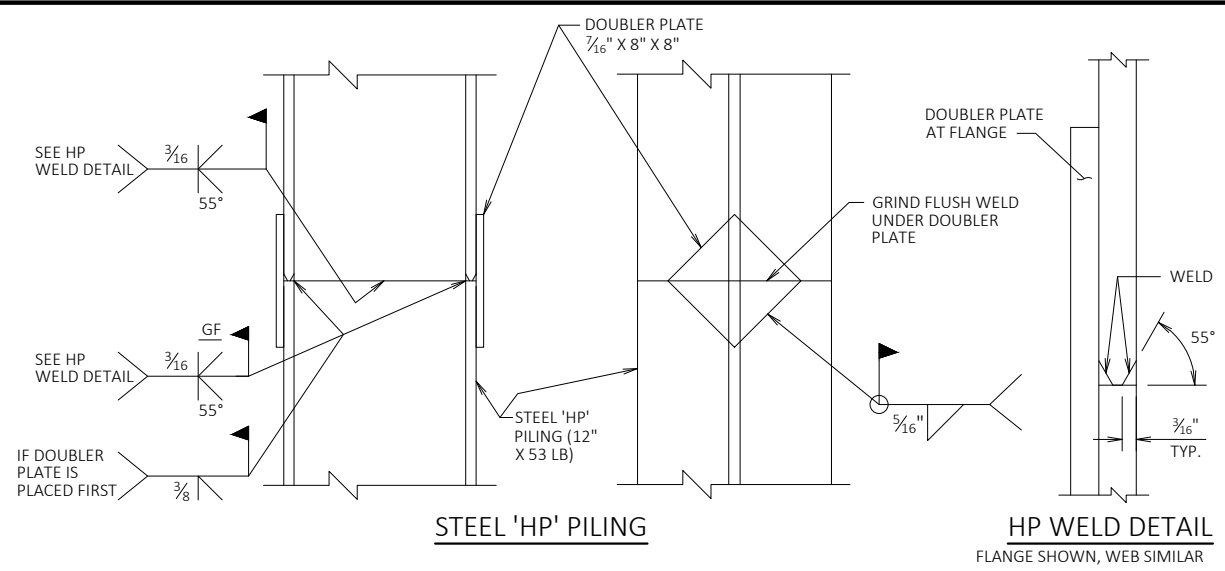
TOTAL ESTIMATED QUANTITIES

BID NUMBER	BID ITEM	UNIT	WEST APPR. SLAB	WEST ABUT.	PIER 1	PIER 2	EAST ABUT.	EAST APPR. SLAB	SUPER	TOTALS
203.0260	REMOVING STRUCTURE OVER WATERWAY MINIMAL DEBRIS B-51-1	EACH	----	----	----	----	----	----	----	1
206.1001	EXCAVATION FOR STRUCTURES BRIDGES B-51-154	EACH	----	----	----	----	----	----	----	1
206.5001	COFFERDAMS B-51-154	EACH	----	----	----	----	----	----	----	2
210.1500	BACKFILL STRUCTURE TYPE A	TON	----	130	----	----	500	----	----	630
305.0120	BASE AGGREGATE DENSE 1 1/4-INCH	TON	155	----	----	----	155	----	----	310
502.0100	CONCRETE MASONRY BRIDGES	CY	71.2	61.6	65.5	65.5	146.4	71.2	746.6	1,228
502.3200	PROTECTIVE SURFACE TREATMENT	SY	100	----	----	----	100	810	----	1,010
502.3210	PIGMENTED SURFACE SEALER	SY	5	----	----	----	5	45	----	55
502.9000.S	UNDERWATER SUBSTRUCTURE INSPECTION B-51-154	EACH	----	----	1	1	----	----	----	2
505.0400	BAR STEEL REINFORCEMENT HS STRUCTURES	LB	----	3,300	2,620	2,620	6,270	----	----	14,810
505.0600	BAR STEEL REINFORCEMENT HS COATED STRUCTURES	LB	11,475	2,420	90	90	4,580	11,475	130,310	137,490
505.0800.S	BAR STEEL REINFORCEMENT HS STAINLESS STRUCTURES	LB	225	----	----	----	225	----	----	450
511.1200	TEMPORARY SHORING B-51-154	SF	----	150	----	----	450	----	----	600
513.7016	RAILING STEEL TYPE C3	LF	37	----	----	----	37	338	----	412
513.8016	RAILING STEEL PEDESTRIAN TYPE C3	LF	----	----	----	----	29	----	----	29
516.0500	RUBBERIZED MEMBRANE WATERPROOFING	SY	----	12	----	----	12	----	----	24
517.1010.S	CONCRETE STAINING B-51-154	SF	240	80	180	180	90	240	2,070	3,080
517.1015.S	CONCRETE STAINING MULTI-COLOR B-51-154	SF	----	290	840	840	1,080	----	----	3,050
517.1050.S	ARCHITECTURAL SURFACE TREATMENT B-51-154	SF	----	290	840	840	1,080	----	----	3,050
531.8990	ANCHOR ASSEMBLIES POLES ON STRUCTURES	EACH	----	----	----	----	----	2	----	2
550.0010	PREBORING UNCONSOLIDATED MATERIALS	LF	----	245	182	182	442	----	----	1,051
550.0020	PREBORING ROCK OR CONSOLIDATED MATERIALS	LF	----	105	65	65	130	----	----	365
550.1120	PILING STEEL HP 12-INCH X 53 LB	LF	----	420	390	390	780	----	----	1,980
606.0300	RIPRAP HEAVY	CY	----	80	----	----	40	----	----	120
612.0406	PIPE UNDERDRAIN WRAPPED 6-INCH	LF	----	125	----	----	125	----	----	250
645.0111	GEOTEXTILE TYPE DF SCHEDULE A	SY	----	85	----	----	95	----	----	180
645.0120	GEOTEXTILE TYPE HR	SY	----	160	----	----	100	----	----	260
652.0125	CONDUIT RIGID METALLIC 2-INCH	LF	5	----	----	----	5	40	----	50
652.0225	CONDUIT RIGID NON-METALLIC SCHEDULE 40 2-INCH	LF	100	----	----	----	100	670	----	870
653.0222	JUNCTION BOXES 18x12x6-INCH	EACH	----	----	----	----	----	2	----	2
SPV.0090.03	NW WINGWALL DECORATIVE RAILING	LF	----	27	----	----	----	----	----	27
NON-BID ITEMS										
	FILLER	SIZE	----	----	----	----	----	----	----	1/2", 3/4", 1 1/2"

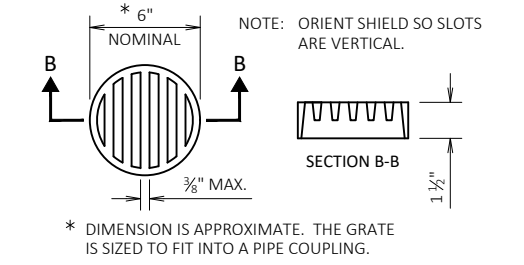
NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-51-154			
DRAWN BY		PKF	PLANS CK'D. BH
ORIGINAL PLAN PREPARED BY CORRE			SHEET 2 OF 28
TYPICAL SECTION, NOTES & QUANTITIES			



PROFILE GRADE LINE - JEFFERSON STREET



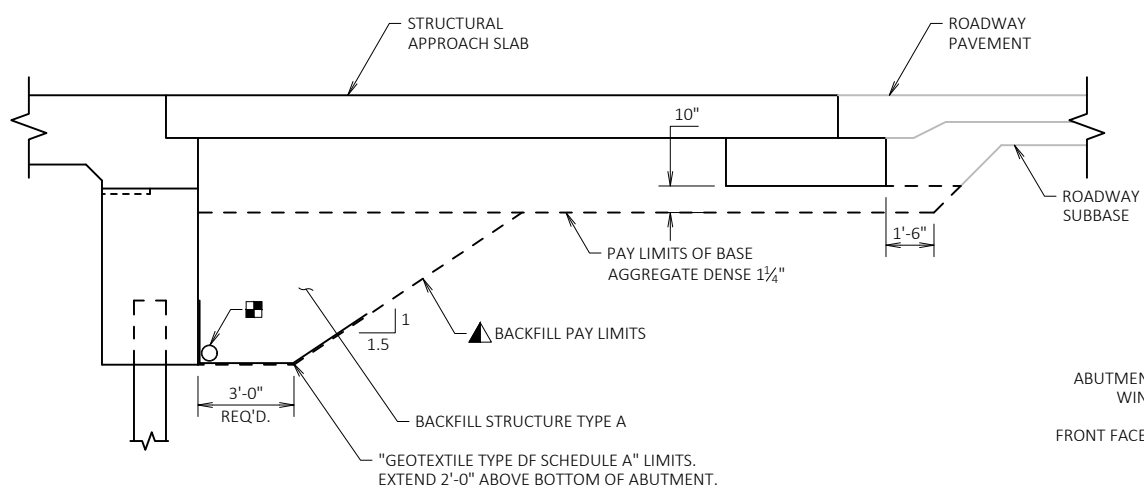
STEEL 'HP' PILING
PILE SPLICE DETAILS



RODENT SHIELD DETAIL

THE RODENT SHIELD, PIPE COUPLING AND SCREWS SHALL BE CONSIDERED INCIDENTAL TO THE BID ITEM "PIPE UNDERDRAIN WRAPPED 6-INCH".

THE RODENT SHIELD SHALL BE A PVC GRATE SIMILAR TO THIS DETAIL. THE GRATE IS COMMERCIALY AVAILABLE AS A FLOOR STRAINER. A PIPE COUPLING IS REQUIRED FOR THE ATTACHMENT OF THIS SHIELD TO THE OUTFALL PIPE. THE SHIELD SHALL BE FASTENED TO THE PIPE COUPLING WITH TWO OR MORE NO. 10 X 1-INCH SCREWS.

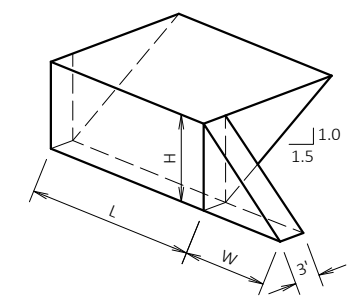


STRUCTURE BACKFILL LIMITS

PIPE UNDERDRAIN WRAPPED (6-INCH). SLOPE 0.5% MIN. TO SUITABLE DRAINAGE AS DETERMINED BY ENGINEER. ATTACH RODENT SHIELD AT ENDS OF PIPE UNDERDRAIN.

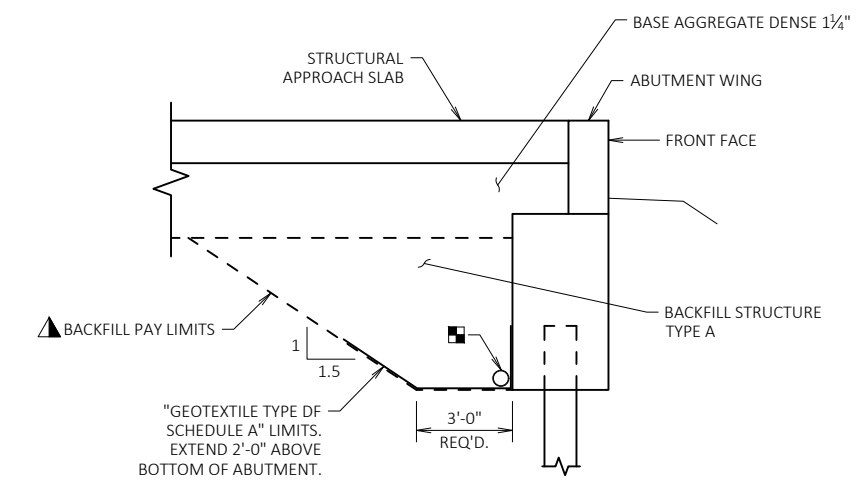
BACKFILL BEYOND PAY LIMITS SHALL BE INCIDENTAL TO EXCAVATION FOR STRUCTURES. LIMITS OF EXCAVATION SHALL BE DETERMINED BY THE CONTRACTOR.

NOTE: EXCAVATION BELOW THE ABUTMENT AND ABUTMENT BEDDING MATERIALS REQUIRES ENGINEER APPROVAL. GEOTEXTILE SHALL BE SET AT THE BOTTOM OF EXCAVATION AND EXTEND 2'-0" ABOVE BOTTOM OF ABUTMENT.

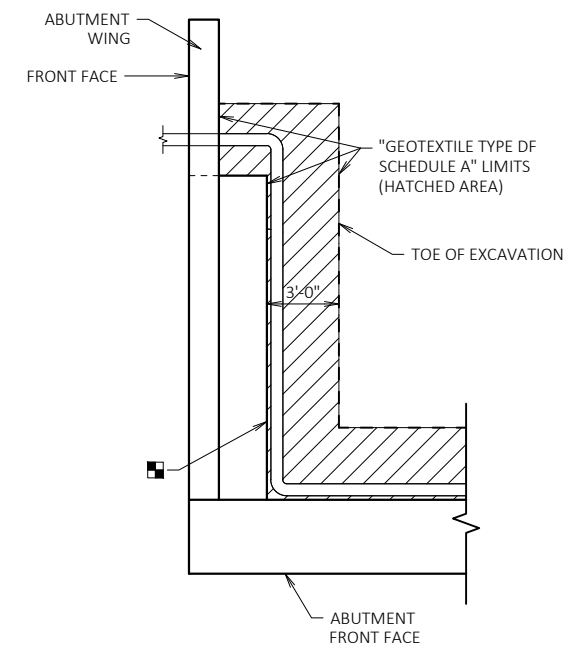


ABUTMENT BACKFILL DIAGRAM

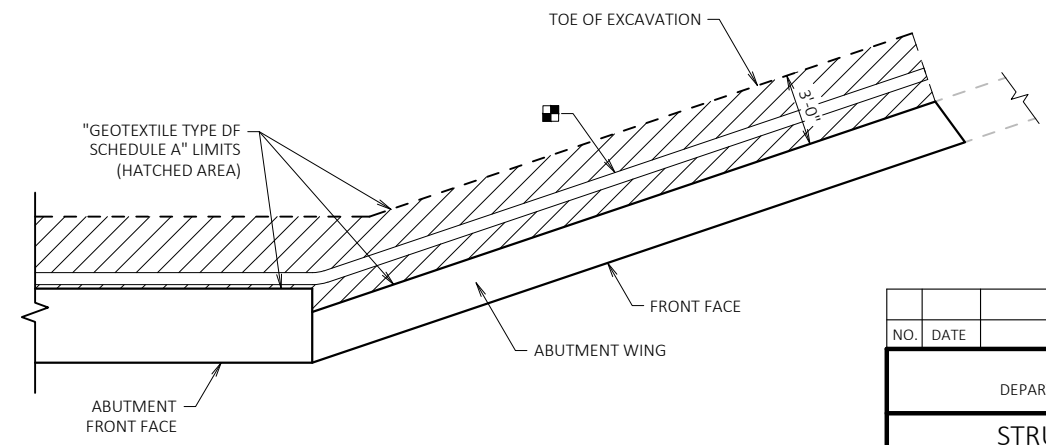
- L = OUT TO OUT OF ABUTMENT BODY (FT)
- H = AVERAGE ABUTMENT FILL HEIGHT (FT)
- W = WING LENGTH (FT)
- EF = EXPANSION FACTOR (1.20 FOR CY BID ITEMS AND 1.00 FOR TON BID ITEMS)
- $V_{CF} = (L)(3.0')(H) + (L)(0.5)(1.5H)(H) + (3.0' \times 0.5)(W)(H)$
- $V_{CY} = V_{CF} / 27$
- $V_{TON} = V_{CY} (2.0)$



TYPICAL SECTION THRU WING



ABUTMENT PLAN WITH WING
PARALLEL TO ROAD



ABUTMENT PLAN WITH
SKEWED WING

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-51-154			
DRAWN BY		PKF	PLANS CK'D. BH
MISCELLANEOUS DETAILS			SHEET 3 of 28



BORING #	DATE COMPLETED	NORTHING (Y)	EASTING (X)
1	MAY 19, 2016	169014.1	508573.8
2	MAY 19, 2016	168955.1	508449.4
3	MAY 20, 2016	168954.5	508512.3
4	MAY 20, 2016	169011.5	508629.6

BORINGS COMPLETED BY: GESTRA ENGINEERING, INC.
 REPORT COMPLETED BY: GESTRA ENGINEERING, INC.
 ALL COORDINATES REFERENCED TO WCCS NAD 83(91) RACINE COUNTY

STATE PROJECT NUMBER

3834-00-72

MATERIAL SYMBOLS

	ASPHALT		TOPSOIL		PEAT
	CONCRETE		FILL		GRAVEL
	SAND		CLAY		SILT
	BOULDERS OR COBBLES		LIMESTONE		BEDROCK (UNKNOWN)
	SHALE		SANDSTONE		IGNEOUS/META

LEGEND OF BORING

ST (1) (2)
0.25 17

F-C COBBLE OR BOULDER
WEATHERED LIMESTONE
CORE RUN #1 - 24'-29'
REC=80%, RQD=72%

- (1) UNCONFINED STRENGTH, AS DETERMINED BY A POCKET PENETROMETER (TSF)
 (2) UNLESS OTHERWISE SPECIFIED THE SPT 'N' VALUE IS BASED ON AASHTO T-206, STANDARD PENETRATION TEST. THE SPT 'N' VALUE PRESENTED HAS NOT BEEN CORRECTED FOR OVERBURDEN PRESSURE OR HAMMER EFFICIENCY.

GROUND WATER ELEVATION

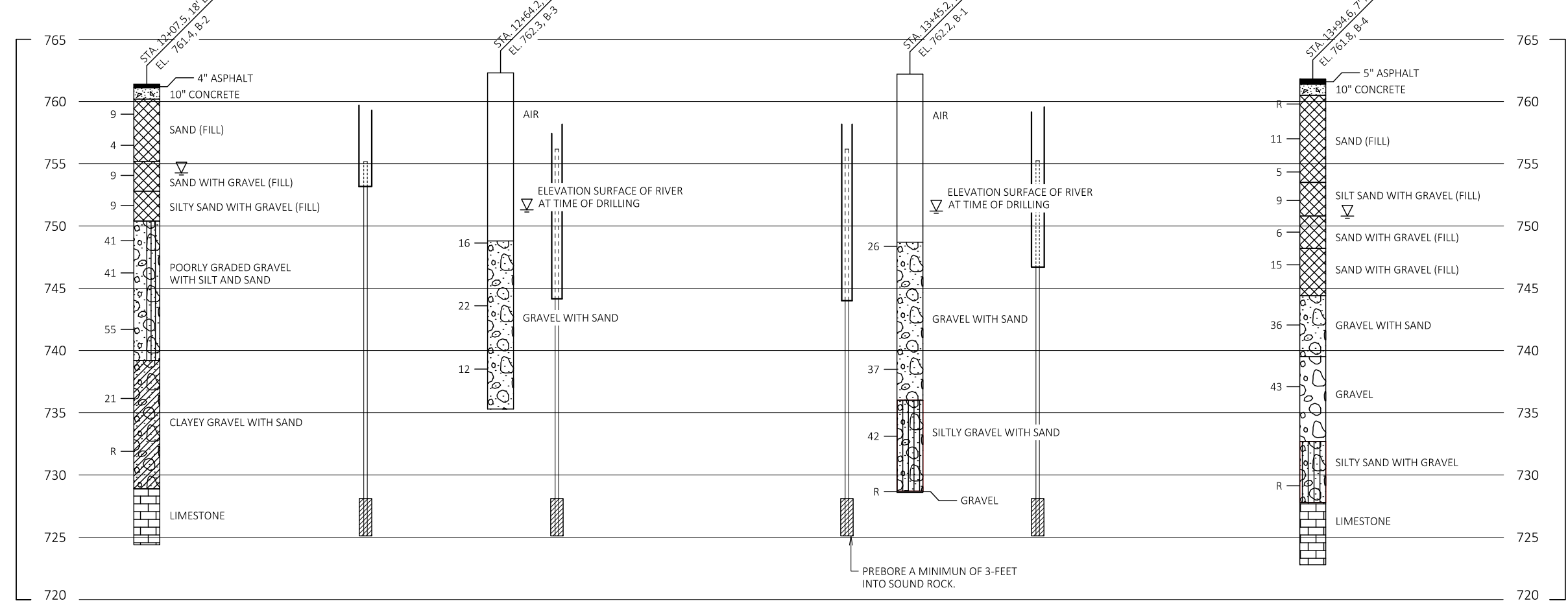
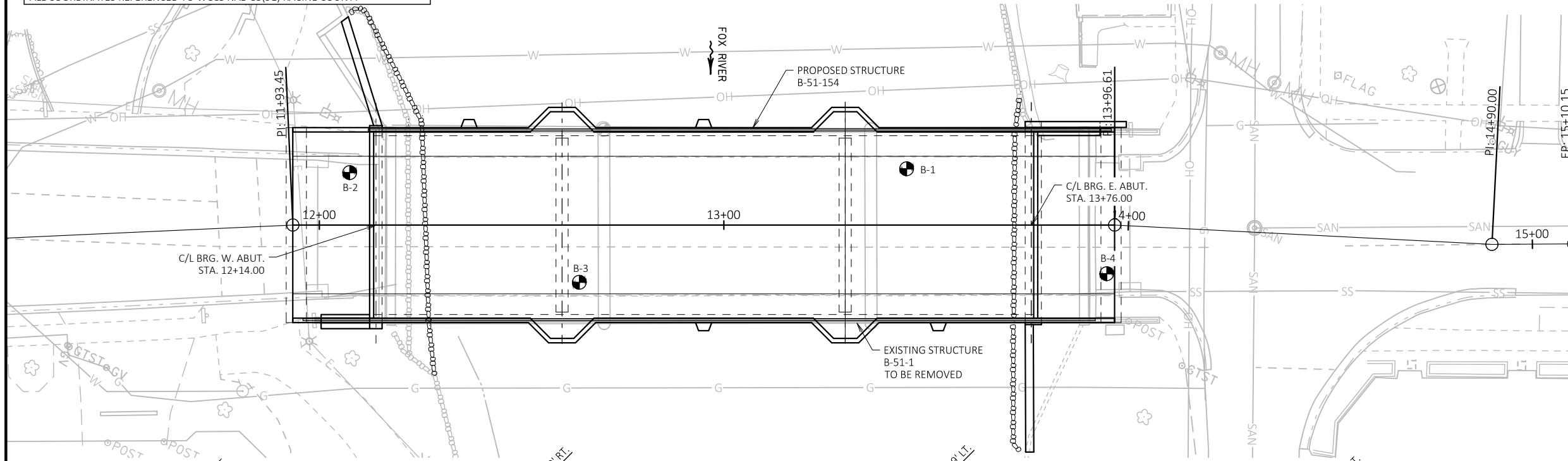
- ▽ AT TIME OF DRILLING
- ▼ END OF DRILLING
- ▼ AFTER DRILLING

ABBREVIATIONS

F-FINE M-MEDIUM C-COARSE ST-SHELBY TUBE

SUBSURFACE EXPLORATION FOR FOUNDATION DESIGN AND BIDDERS INFORMATION

BORINGS WERE COMPLETED AT POINTS APPROXIMATELY AS INDICATED ON THIS DRAWING TO OBTAIN INFORMATION CONCERNING THE CHARACTER OF SUBSURFACE MATERIALS FOUND AT THE SITE. BECAUSE THE INVESTIGATED DEPTHS ARE LIMITED AND THE AREA OF THE BORINGS IS VERY SMALL IN RELATION TO THE ENTIRE SITE, THE WISCONSIN DEPARTMENT OF TRANSPORTATION DOES NOT WARRANT SIMILAR SUBSURFACE CONDITIONS BELOW, BETWEEN, OR BEYOND THESE BORINGS. VARIATIONS IN SOIL CONDITIONS SHOULD BE EXPECTED AND FLUCTUATIONS IN GROUNDWATER LEVELS MAY OCCUR.



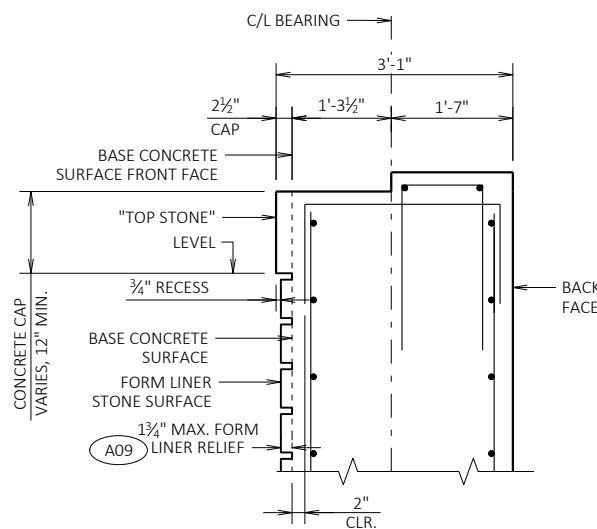
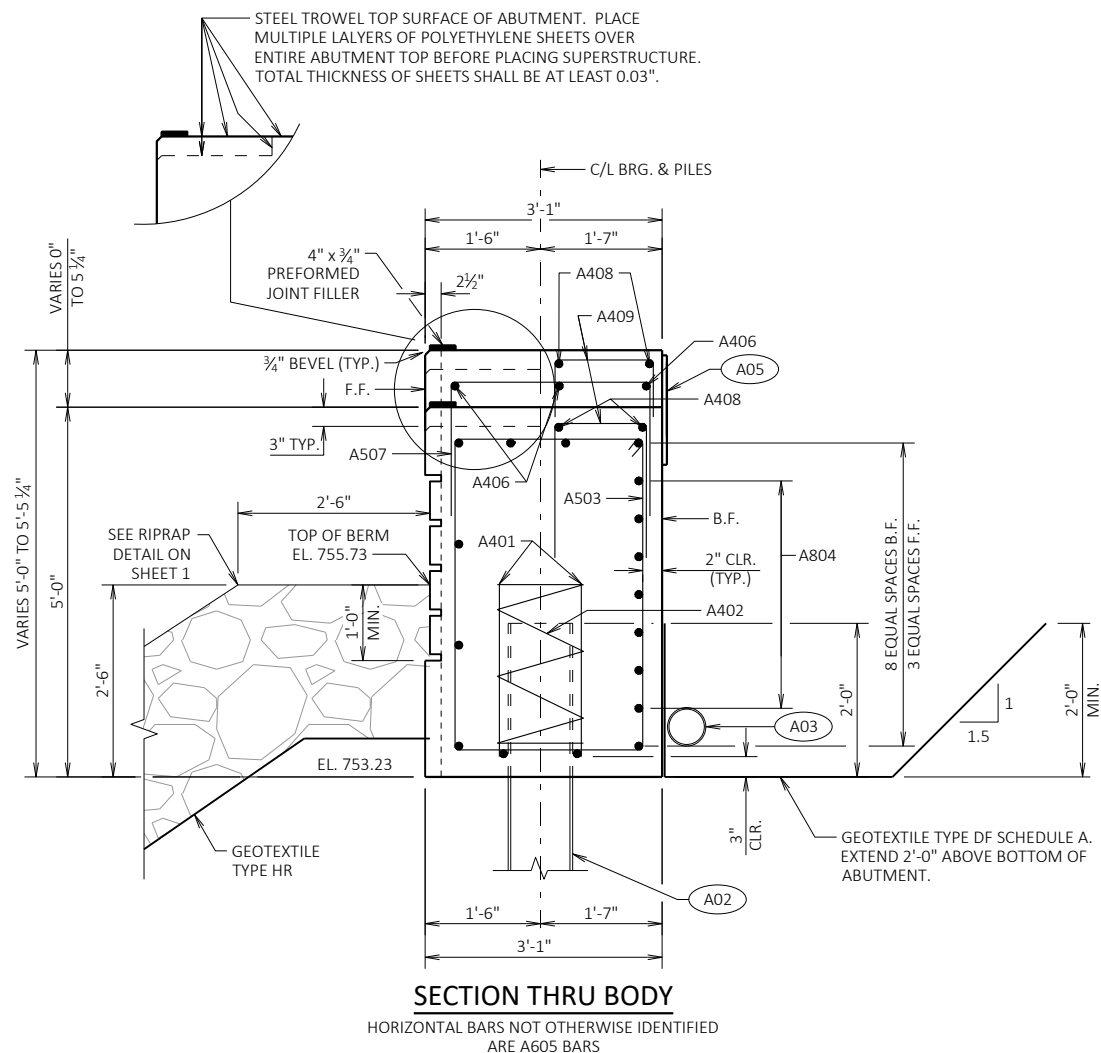
NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

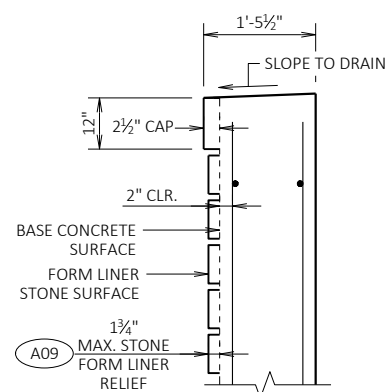
STRUCTURE B-51-154

DRAWN BY PKF PLANS CK'D. BH

SUBSURFACE EXPLORATION SHEET 4 OF 28



TYPICAL ABUTMENT SECTION WITH STONE FORM LINER



TYPICAL WINGWALL SECTION WITH STONE FORM LINER (APPLIES TO WING #1)

BILL OF BARS - WEST ABUTMENT

BAR MARK	NO. REQ'D	LENGTH	BENT	BAR SERIES	LOCATION
NON-COATED BARS TOTAL WEIGHT = 3,300 LBS					
A401	14	2'-3"			BODY @ PILING VERT.
A402	7	28'-0"	X		BODY @ PILING VERT.
A503	62	14'-4"	X		BODY - STIRRUPS VERT.
A804	14	28'-8"	X		BODY - B.F. HORIZ.
A605	20	26'-5"			BODY - F.F., TOP, & BTM. HORIZ.
A406	3	14'-3"			BODY - TOP HORIZ.
A507	21	5'-3"	X		BODY - TOP VERT.
A408	6	18'-0"			BODY - TOP @ NOTCH HORIZ.
A409	30	4'-7"	X		BODY - TOP @ NOTCH VERT.
A510	8	2'-0"			BODY @ STORM SEWER VERT.
A411	8	4'-7"			ABUTMENT ENDS VERT.
A412	2	2'-0"			ABUTMENT ENDS HORIZ.
COATED BARS TOTAL WEIGHT = 2,420 LBS					
A520	13	15'-10"	X		WING 1 - BOTTOM, STIRRUPS VERT.
A521	6	14'-6"			WING 1 - BOTTOM, F.F. HORIZ.
A622	8	14'-6"			WING 1 - BOTTOM, B.F., & TOP HORIZ.
A523	21	10'-0"	X		WING 1 - TOP VERT.
A424	8	14'-6"			WING 1 - TOP HORIZ.
A430	40	15'-1"			WING 2 - F.F. & B.F. HORIZ.
A431	2	21'-6"			WING 2 - F.F. & B.F. HORIZ.
A432	4	15'-2"			WING 2 - F.F. & B.F. HORIZ.
A533	74	9'-11"		X	WING 2 - F.F. & B.F. VERT.
A534	1	9'-7"			WING 2 - F.F. VERT.
A535	1	10'-4"			WING 2 - F.F. VERT.
A436	63	2'-7"	X		WING 2 - ADJACENT TO PILING HORIZ.
A537	37	5'-5"	X		WING 2 - TOP VERT.
A438	23	4'-6"	X		WING 2 - SIDES VERT.

THE FIRST DIGIT OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE. DIMENSIONS IN BENDING DETAILS ARE OUT-TO-OUT OF BAR. LENGTH SHOWN FOR BAR SERIES IS AN AVERAGE. SEE BAR SERIES TABLE FOR ACTUAL LENGTHS.

BAR SERIES TABLE

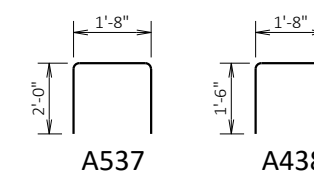
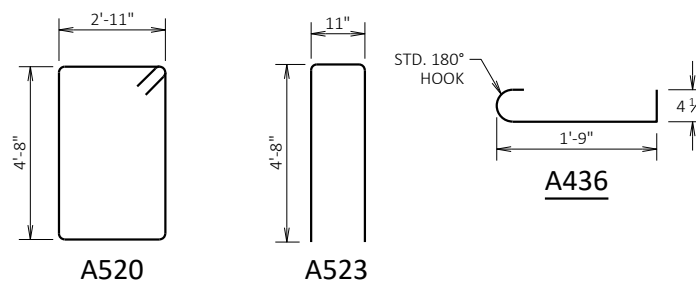
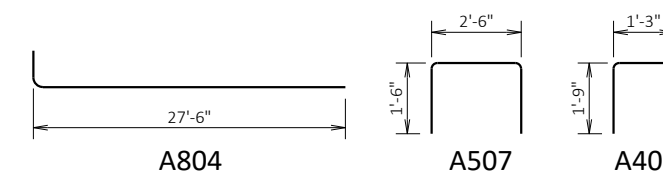
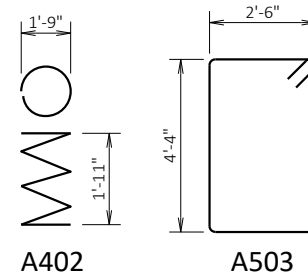
BAR MARK	NO. REQ'D.	LENGTH
A533	2 SERIES OF 37	9'-7" TO 10'-3"

BUNDLE AND TAG EACH SERIES SEPARATELY.

LEGEND

- A02 SUPPORT ABUTMENT ON PILING STEEL HP 12-INCH x 53 LB, PREBORED A MINIMUM OF 3' INTO BEDROCK. ESTIMATED 30' LONG.
- A03 PIPE UNDERDRAIN WRAPPED 6-INCH. SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. ATTACH RODENT SCREEN AT ENDS OF PIPE UNDERDRAIN. RODENT SHIELD TO BE INCLUDED IN BID PRICE OF "PIPE UNDERDRAIN WRAPPED 6-INCH".
- A05 18" RUBBERIZED MEMBRANE WATERPROOFING. SEAL ALL HORIZONTAL AND VERTICAL JOINTS ON BACKFACE.
- A09 STONE FORMLINER ON THE EXPOSED SURFACES OF THE FRONT FACE OF ABUTMENT AND WING (WHERE SHOWN). EXTEND FORMLINER 1'-0" BELOW GROUNDLINE.

B.F. DENOTES BACK FACE
F.F. DENOTES FRONT FACE



NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-51-154			
DRAWN BY PKF		PLANS CK'D. BH	
WEST ABUTMENT - 3			SHEET 7 OF 28



LEGEND

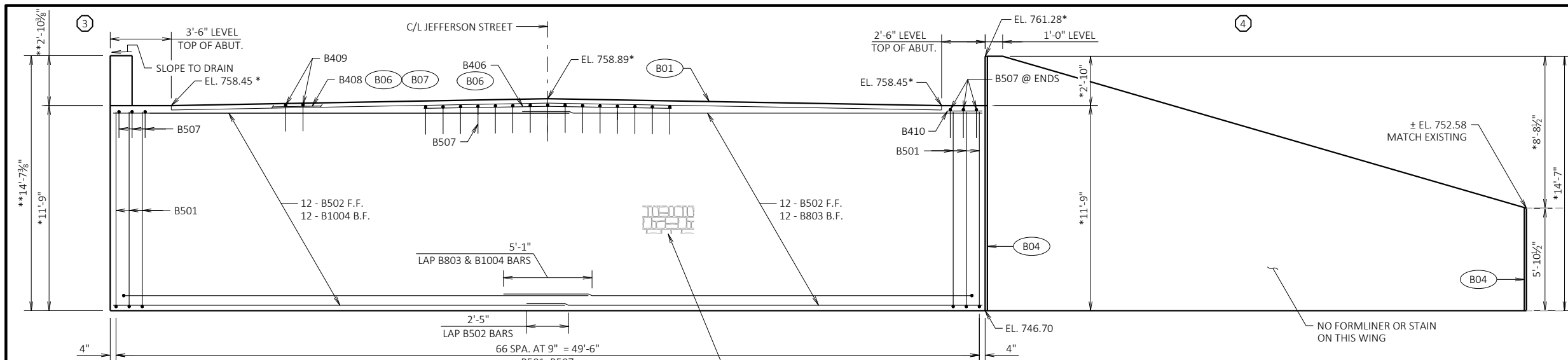
- * THE ELEVATIONS OR DIMENSIONS ARE GIVEN AT THE TOP OF CONCRETE AT THE C/L OF BEARING.
- ** DIMENSIONS ARE GIVEN AT THE F.F. OF ABUTMENT AT THE BACK OF WING. FOR WING DETAILS AND ELEVATIONS SEE SHEET 9 & 10.

- INDICATES WING NUMBER
- B01 STEEL TROWEL TOP SURFACE OF ABUTMENT. PLACE MULTIPLE LAYERS OF POLYETHYLENE SHEETS OVER ENTIRE ABUTMENT TOP BEFORE PLACING SUPERSTRUCTURE. TOTAL THICKNESS OF SHEETS SHALL BE AT LEAST 0.03".
- B02 SUPPORT ABUTMENT ON PILING STEEL HP 12-INCH x 53 LB, PREBORED A MINIMUM OF 3" INTO BEDROCK. ESTIMATED 30' LONG. PILES PLACED IN PREBORED HOLES BORED INTO ROCK DO NOT REQUIRE DRIVING. PILING SHALL BE FIRMLY SEATED AFTER PLACEMENT.
- B03 PIPE UNDERDRAIN WRAPPED 6-INCH. SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. ATTACH RODENT SCREEN AT ENDS OF PIPE UNDERDRAIN. RODENT SHIELD TO BE INCLUDED IN BID PRICE OF "PIPE UNDERDRAIN WRAPPED 6-INCH".
- B04 1/2" FILLER (INCLUDED IN WING LENGTH): SEAL ALL EXPOSED HORIZONTAL AND VERTICAL SURFACES OF 1/2" FILLER WITH NON-STAINING, NON-BITUMINOUS JOINT SEALER. (1" DEEP AND HOLD 1/8" BELOW SURFACE OF CONCRETE). COLOR TO MATCH ADJACENT SURFACE.
- B05 18" RUBBERIZED MEMBRANE WATERPROOFING. SEAL ALL HORIZONTAL AND VERTICAL JOINTS ON BACKFACE.
- B06 FIELD BEND TO FOLLOW SLOPE OF TOP OF ABUTMENT.
- B07 LAP BARS 1'-11" MIN., 2 LAPS PER ROW.
- B08 COORDINATE WITH DESIGN ENGINEER IF PILE LOCATIONS NEED TO BE ADJUSTED TO MISS GAS MAIN.
- B09 3/4" CORK FILLER ON VERTICAL FACES PARALLEL TO SLAB EDGE.

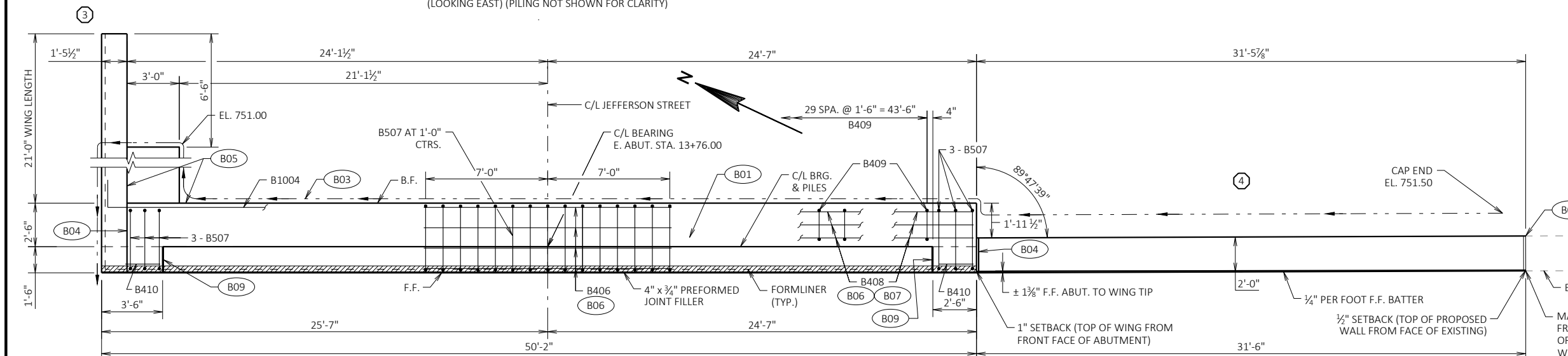
B.F. DENOTES BACK FACE
F.F. DENOTES FRONT FACE

NOTES

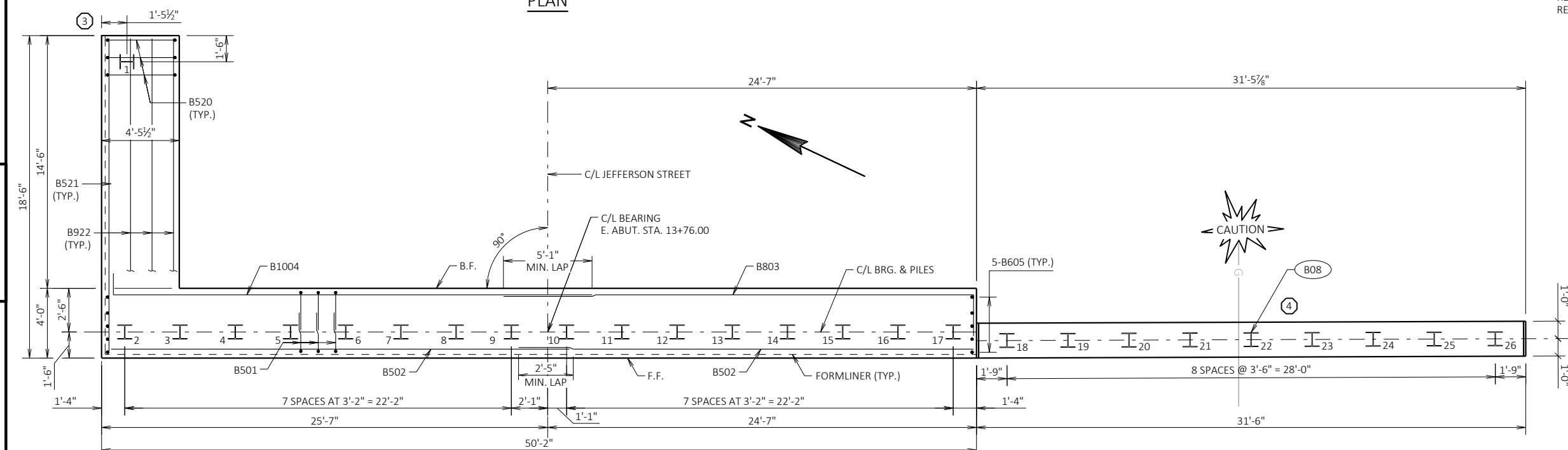
DO NOT PLACE FILL MORE THAN 3'-0" FROM BOTTOM OF ABUTMENT UNTIL SUPERSTRUCTURE IS IN PLACE.



ELEVATION
(LOOKING EAST) (PILING NOT SHOWN FOR CLARITY)

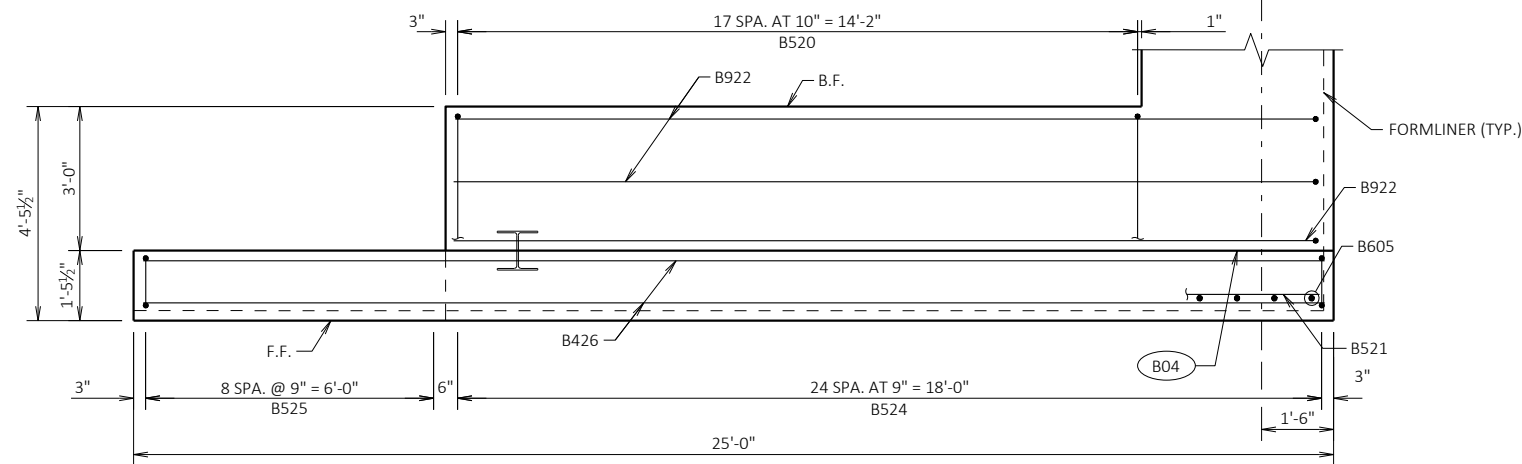


PLAN

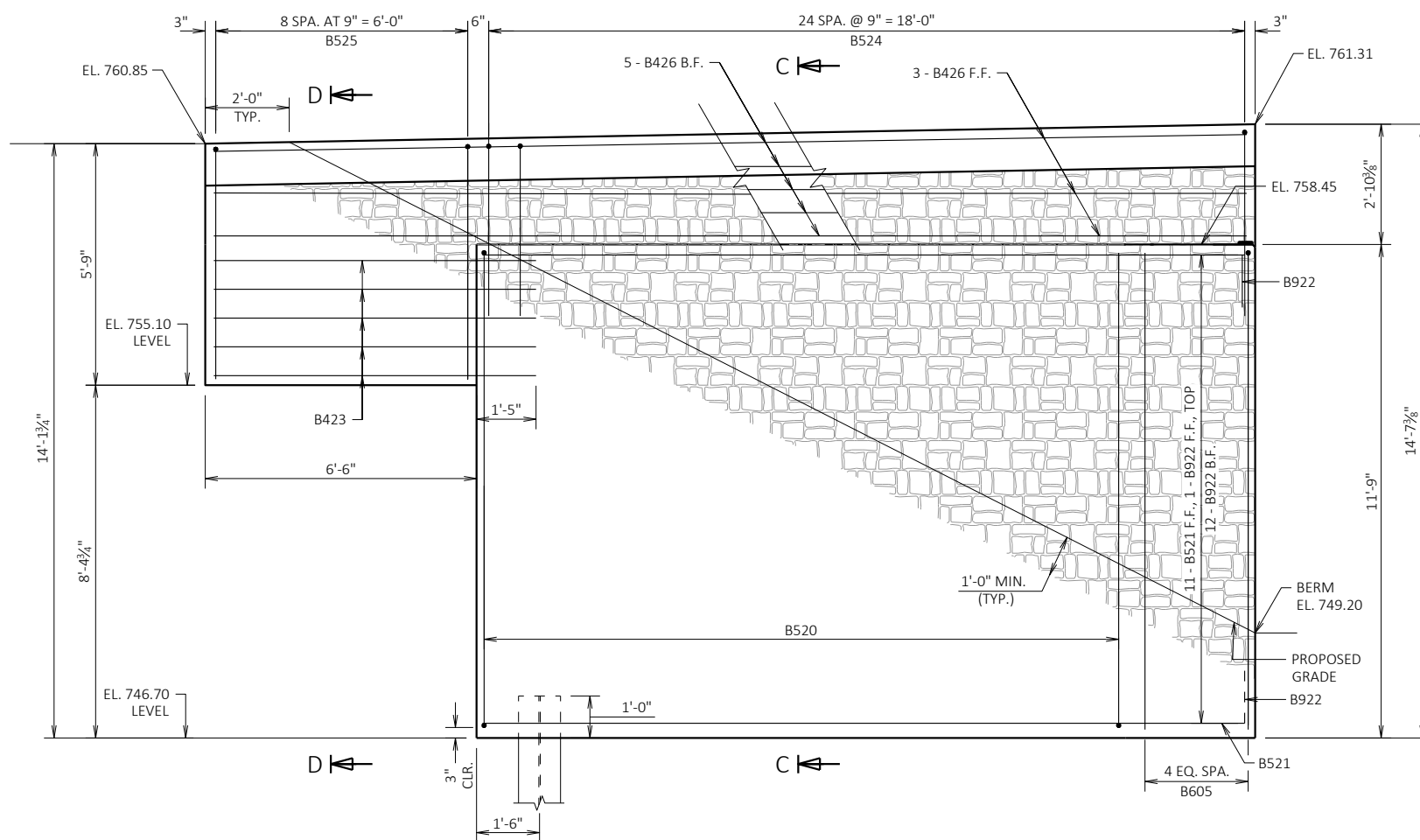


PILE PLAN

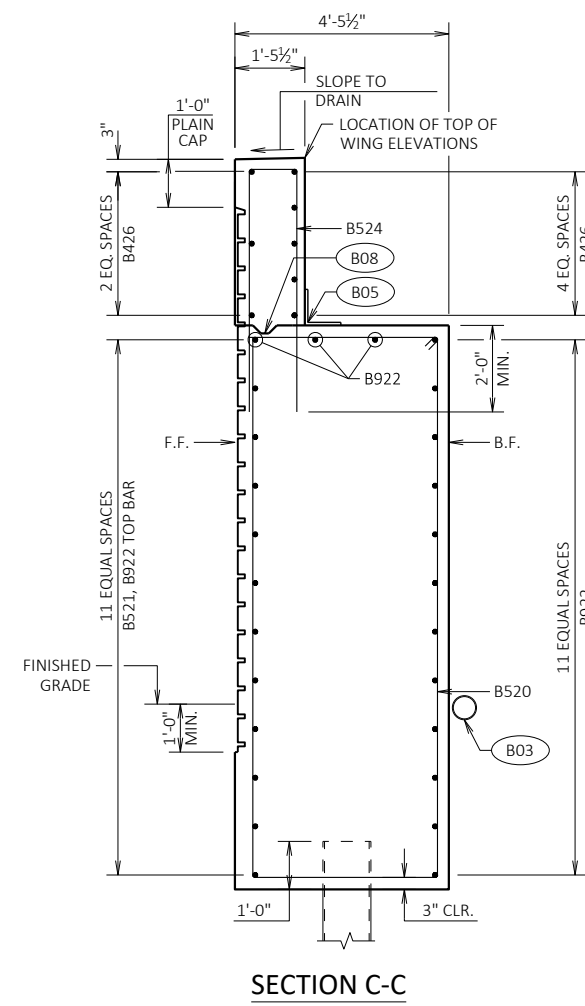
NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-51-154			
DRAWN BY PKF		PLANS CK'D. BH	
EAST ABUTMENT - 1			SHEET 8 OF 28



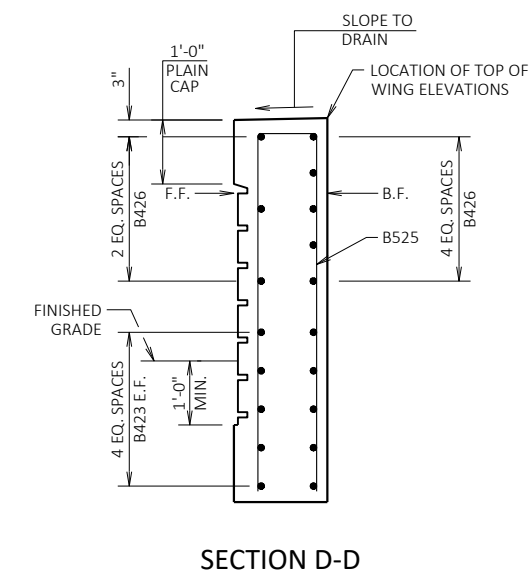
WING 3 PLAN



WING 3 ELEVATION



SECTION C-C



SECTION D-D

LEGEND

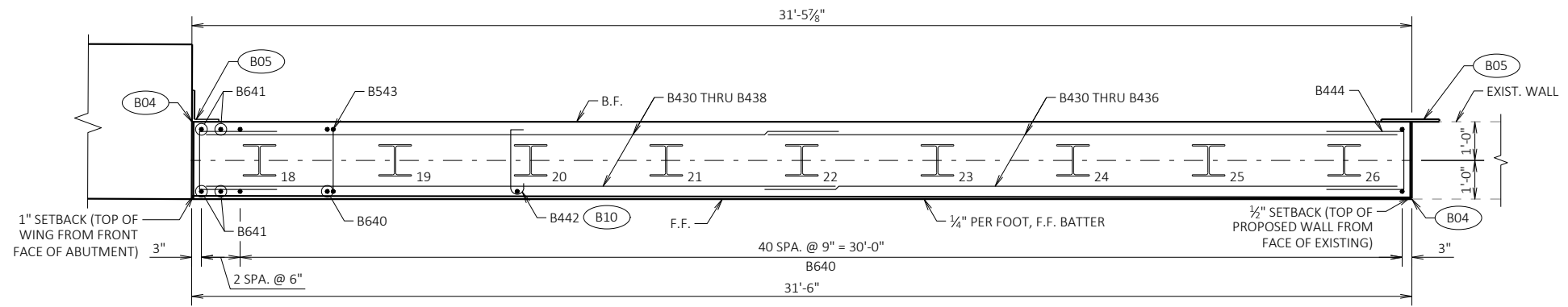
- (B03) PIPE UNDERDRAIN WRAPPED 6-INCH. SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. ATTACH RODENT SCREEN AT ENDS OF PIPE UNDERDRAIN. RODENT SHIELD TO BE INCLUDED IN BID PRICE OF "PIPE UNDERDRAIN WRAPPED 6-INCH".
 - (B04) 1/2" FILLER: SEAL ALL EXPOSED HORIZONTAL AND VERTICAL SURFACES OF 1/2" FILLER WITH NON-STAINING, NON-BITUMINOUS JOINT SEALER. (1" DEEP AND HOLD 1/8" BELOW SURFACE OF CONCRETE). COLOR TO MATCH ADJACENT SURFACE.
 - (B05) 18" RUBBERIZED MEMBRANE WATERPROOFING. SEAL ALL HORIZONTAL AND VERTICAL JOINTS ON BACKFACE.
 - (B08) OPTIONAL KEYED CONST. JOINT FORMED BY BEVELED 2" x 6" (18" RUBBERIZED MEMBRANE WATERPROOFING AT B.F. & 3/4" "V" GROOVE AT F.F. IF JOINT IS USED).
- B.F. DENOTES BACK FACE
F.F. DENOTES FRONT FACE

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-51-154			
DRAWN BY PKF		PLANS CKD. BH	
EAST ABUTMENT - 2			SHEET 9 OF 28

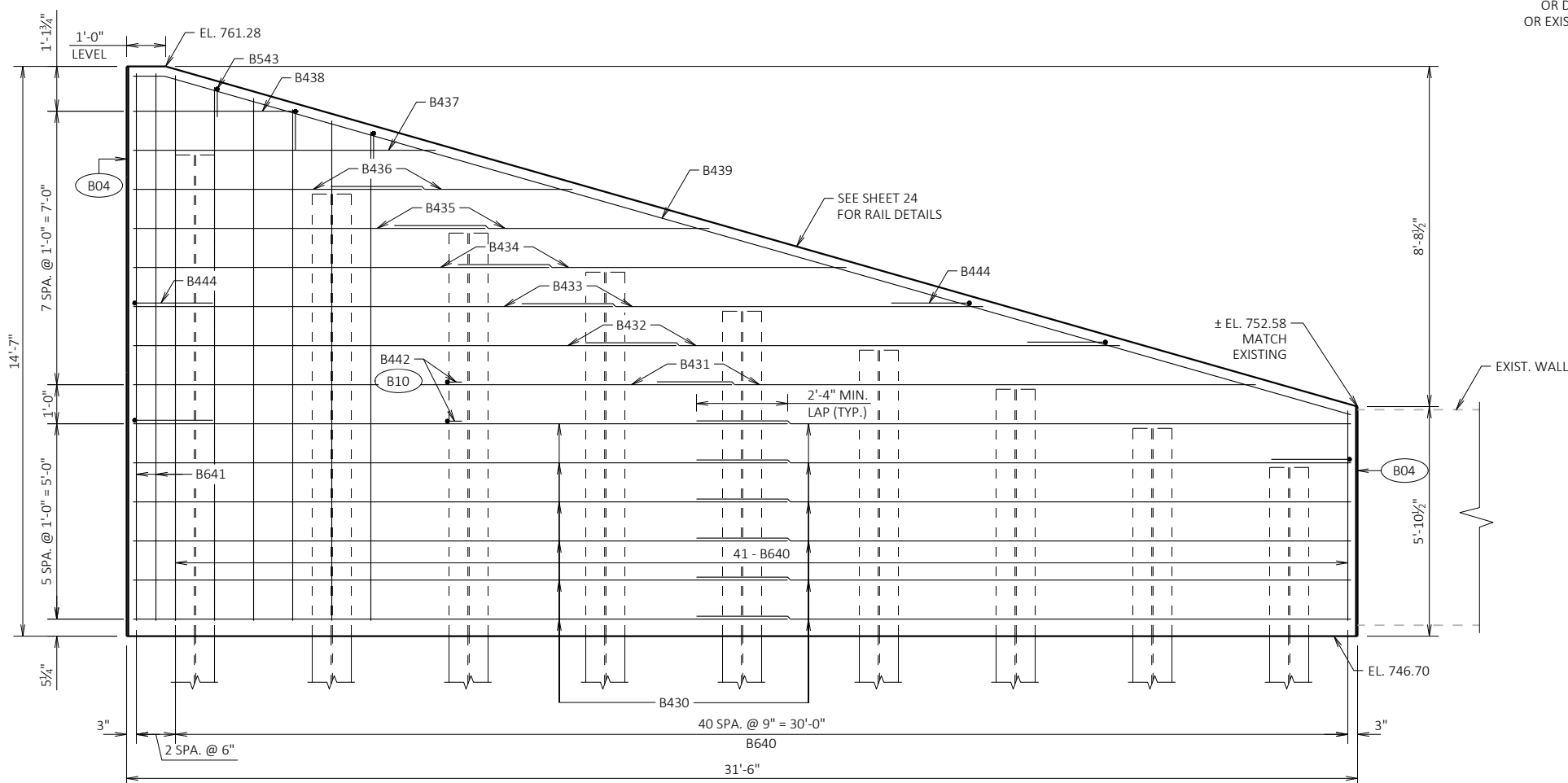


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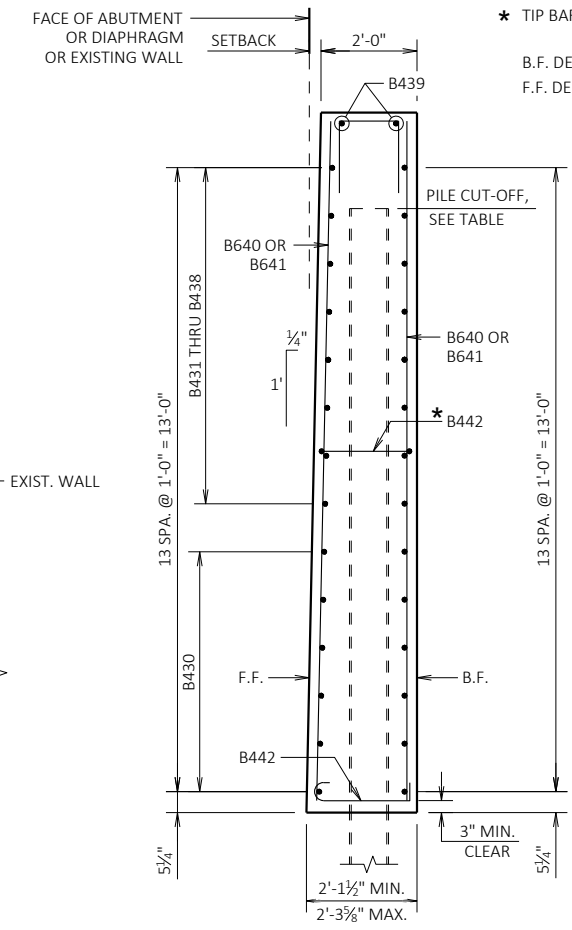
8



WING 4 PLAN



WING 4 ELEVATION



WING 4 SECTION

LEGEND

- (B03) PIPE UNDERDRAIN WRAPPED 6-INCH. SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. ATTACH RODENT SCREEN AT ENDS OF PIPE UNDERDRAIN. RODENT SHIELD TO BE INCLUDED IN BID PRICE OF "PIPE UNDERDRAIN WRAPPED 6-INCH".
 - (B04) 1/2" FILLER (INCLUDED IN WING LENGTH): SEAL ALL EXPOSED HORIZONTAL AND VERTICAL SURFACES OF 1/2" FILLER WITH NON-STAINING, NON-BITUMINOUS JOINT SEALER. (1" DEEP AND HOLD 1/8" BELOW SURFACE OF CONCRETE). COLOR TO MATCH ADJACENT SURFACE.
 - (B05) 18" RUBBERIZED MEMBRANE WATERPROOFING. SEAL ALL HORIZONTAL AND VERTICAL JOINTS ON BACKFACE.
 - (B08) OPTIONAL KEYED CONST. JOINT FORMED BY BEVELED 2" x 6" (18" RUBBERIZED MEMBRANE WATERPROOFING AT B.F. & 3/4" "V" GROOVE AT F.F. IF JOINT IS USED).
 - (B10) LOCATE B442 BARS ADJACENT TO EACH PILE, ON ONE SIDE ONLY. TIE TO NEAREST VERTICAL BAR. ALTERNATE LOCATION OF 90° AND 180° HOOKS AT EACH VERTICAL LAYER OF TIES.
- * TIP BAR WHERE NECESSARY TO MAINTAIN REQUIRED COVER.

B.F. DENOTES BACK FACE
F.F. DENOTES FRONT FACE

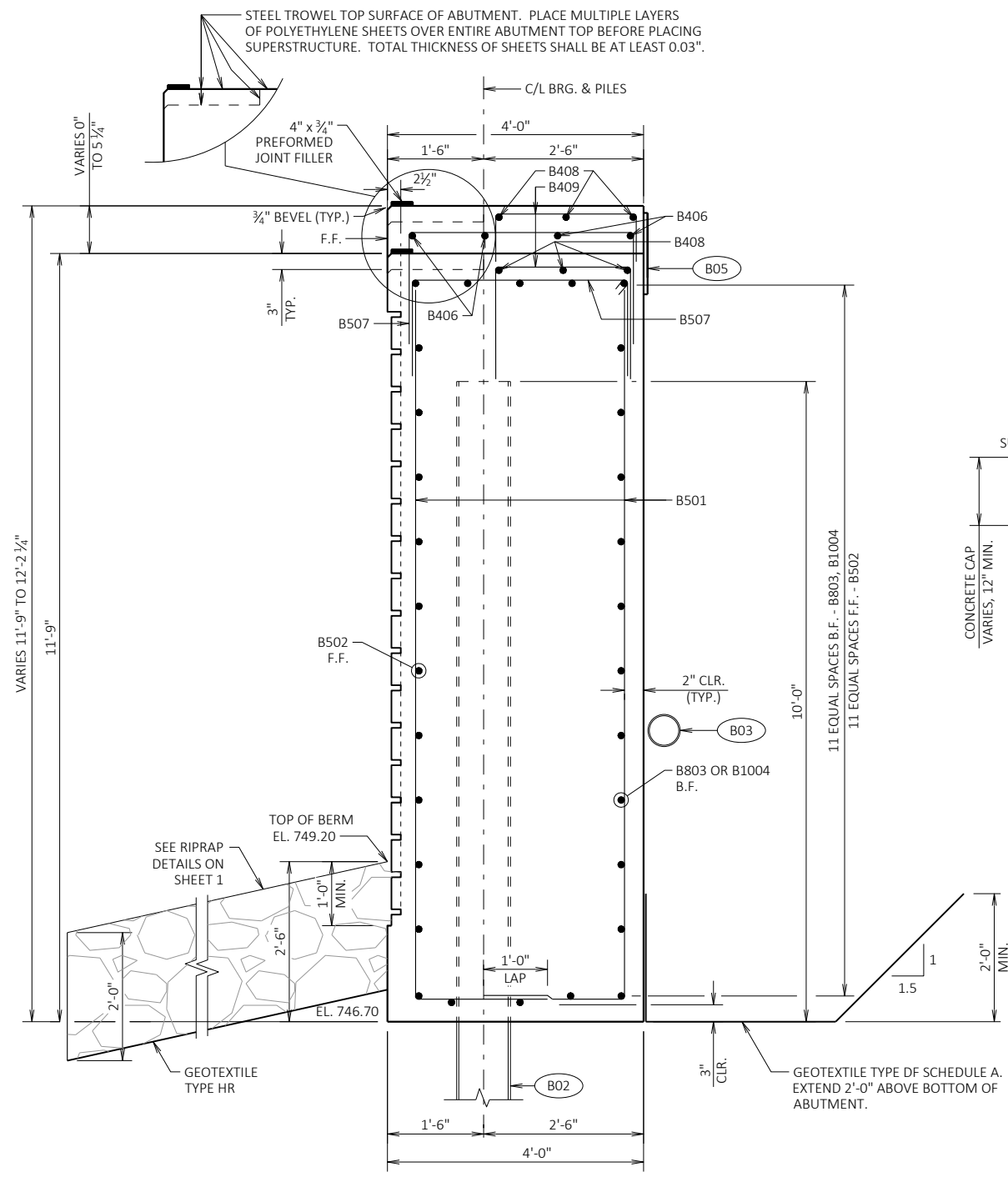
PILE #	PILE CUT-OFF ELEVATIONS
18	758.70
19	757.70
20	756.70
21	755.70
22	754.70
23	753.70
24	752.70
25	751.70
26	750.70

8

8

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-51-154			
DRAWN BY PKF		PLANS CKD. BH	
EAST ABUTMENT - 3			SHEET 10 OF 28





SECTION THRU BODY

HORIZONTAL BARS NOT OTHERWISE IDENTIFIED ARE B502 BARS

BAR SERIES TABLE

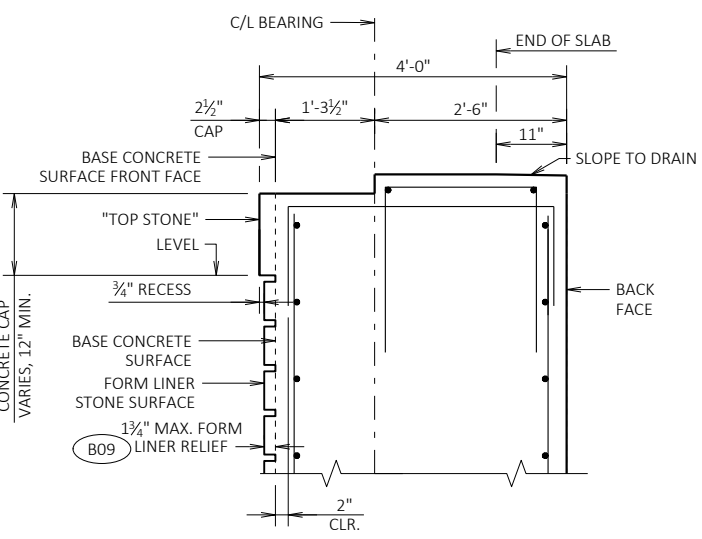
BAR MARK	NO. REQ'D.	LENGTH
B640	2 SERIES OF 41	5'-6" TO 14'-1"

BUNDLE AND TAG EACH SERIES SEPARATELY.

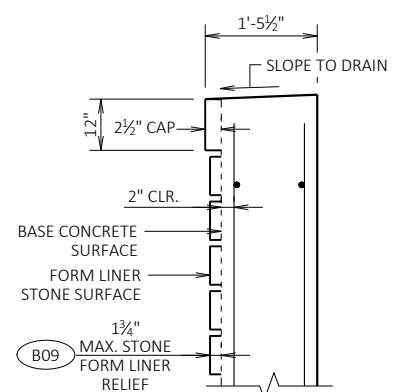
BILL OF BARS - EAST ABUTMENT

BAR MARK	NO. REQ'D	LENGTH	BENT	BAR SERIES	LOCATION
NON-COATED BARS TOTAL WEIGHT = 6,270 LBS					
B501	134	13'-5"	X		BODY F.F. & B.F.
B502	36	26'-2"			BODY - F.F., TOP, & B.F.
B803	12	28'-8"	X		BODY - B.F.
B1004	12	29'-0"	X		BODY - B.F.
B605	10	11'-4"			BODY - ENDS
B406	4	14'-3"			BODY - TOP
B507	88	6'-2"	X		BODY - TOP
B408	9	18'-0"			BODY - TOP
B409	30	5'-6"	X		BODY - TOP
B410	2	2'-0"			BODY - END
COATED BARS TOTAL WEIGHT = 4,580 LBS					
B520	18	31'-6"	X		WING 3 - BOTTOM, STIRRUPS
B521	11	17'-11"			WING 3 - BOTTOM, F.F.
B922	15	19'-0"	X		WING 3 - BOTTOM, B.F., & TOP
B423	10	7'-9"			WING 3 - TOP
B524	25	10'-2"	X		WING 3 - TOP
B525	9	11'-4"	X		WING 3 - TOP
B426	8	24'-6"			WING 3 - TOP
B430	24	16'-9"			WING 4 - F.F. & B.F.
B431	4	15'-7"			WING 4 - F.F. & B.F.
B432	4	13'-10"			WING 4 - F.F. & B.F.
B433	4	12'-1"			WING 4 - F.F. & B.F.
B434	4	10'-4"			WING 4 - F.F. & B.F.
B435	4	8'-7"			WING 4 - F.F. & B.F.
B436	4	6'-10"			WING 4 - F.F. & B.F.
B437	2	7'-9"			WING 4 - F.F. & B.F.
B438	2	4'-3"			WING 4 - F.F. & B.F.
B439	2	32'-4"	X		WING 4 - TOP
B640	82	9'-10"		X	WING 4 - F.F. & B.F.
B641	4	14'-3"			WING 4 - F.F. & B.F.
B442	72	2'-8"	X		WING 4 - ADJACENT TO PILING
B543	43	5'-5"	X		WING 4 - TOP
B444	28	4'-6"	X		WING 4 - ENDS

THE FIRST DIGIT OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE. DIMENSIONS IN BENDING DETAILS ARE OUT-TO-OUT OF BAR. LENGTH SHOWN FOR BAR SERIES IS AN AVERAGE. SEE BAR SERIES TABLE FOR ACTUAL LENGTHS.

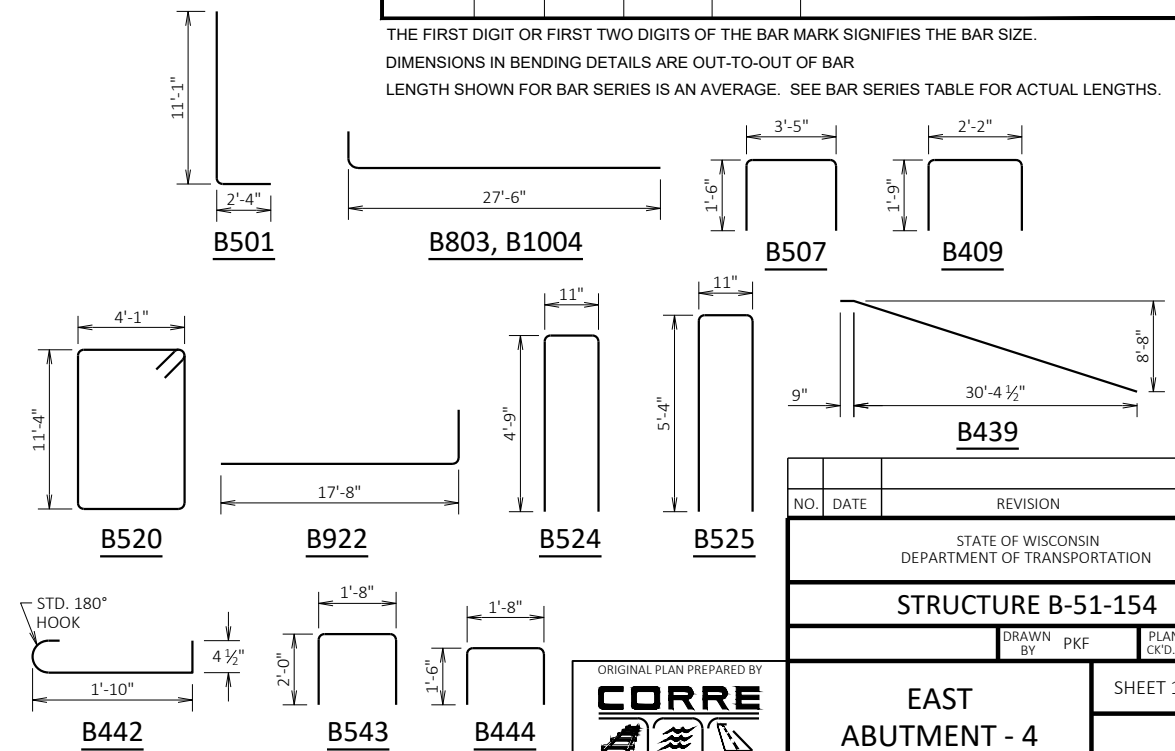


TYPICAL ABUTMENT SECTION WITH STONE FORM LINER



TYPICAL WINGWALL SECTION WITH STONE FORM LINER

(APPLIES TO WING #3)



- LEGEND**
- B02 SUPPORT ABUTMENT ON PILING STEEL HP 12-INCH x 53 LB, PREBORED A MINIMUM OF 3' INTO BEDROCK. ESTIMATED 30' LONG.
 - B03 PIPE UNDERDRAIN WRAPPED 6-INCH. SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. ATTACH RODENT SCREEN AT ENDS OF PIPE UNDERDRAIN. RODENT SHIELD TO BE INCLUDED IN BID PRICE OF "PIPE UNDERDRAIN WRAPPED 6-INCH".
 - B05 18" RUBBERIZED MEMBRANE WATERPROOFING. SEAL ALL HORIZONTAL AND VERTICAL JOINTS ON BACKFACE.
 - B09 STONE FORMLINER ON THE EXPOSED SURFACES OF THE FRONT FACE OF ABUTMENT AND WING (WHERE SHOWN). EXTEND FORMLINER 1'-0" BELOW GROUNDLINE.
- B.F. DENOTES BACK FACE
F.F. DENOTES FRONT FACE

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-51-154			
DRAWN BY		PKE	PLANS CK'D. BH
EAST ABUTMENT - 4			SHEET 11 OF 28

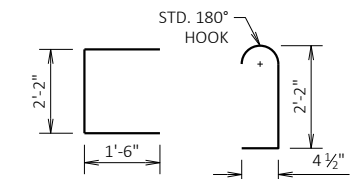
BILL OF BARS - PIER 1

BAR MARK	NO. REQ'D	LENGTH	BENT	BAR SERIES	LOCATION
NON-COATED BARS					
					TOTAL WEIGHT = 2,620 LBS
P501	94	13'-1"			SHAFT
P402	28	42'-2"			SHAFT
P403	28	5'-0"	X		SHAFT - ENDS
P404	156	3'-0"	X		SHAFT - TIE
P405	3	11'-0"			SHAFT - TOP
P506	22	4'-10"	X		SHAFT - TOP
COATED BARS					
					TOTAL WEIGHT = 90 LBS
P507	42	2'-0"			SHAFT - TOP
					VERT.

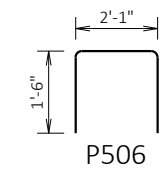
THE FIRST DIGIT OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE.
DIMENSIONS IN BENDING DETAILS ARE OUT-TO-OUT OF BAR

NOTES

COFFERDAM REQUIRED. CONCRETE POURED UNDER WATER WILL BE ALLOWED AND SHALL BE DONE IN ACCORDANCE WITH THE STANDARD SPEC. 502.3.5.3. CONCRETE POURED UNDER WATER SHALL NOT EXCEED 10.0 FEET IN DEPTH, UNLESS APPROVED OTHERWISE.



P403



P506

LEGEND

- P01 2" x 6" BEVELED KEYWAY.
- P02 PIERS SUPPORTED ON PILING STEEL HP 12-INCH x 53 LB PREBORED A MINIMUM OF 3 FEET INTO BEDROCK. ESTIMATED 30' LONG AT THE PIERS. PILES PLACED IN PREBORED HOLES BORED INTO ROCK DO NOT REQUIRE DRIVING. PILING SHALL BE FIRMLY SEATED AFTER PLACEMENT.
- P03 P507 BARS MAY BE PLACED AFTER PIER IS POURED BUT BEFORE CONC. HAS SET. EMBED BARS 1'-0".
- P04 FIELD BEND BARS TO FOLLOW TOP SLOPE.

NO.	DATE	REVISION	BY

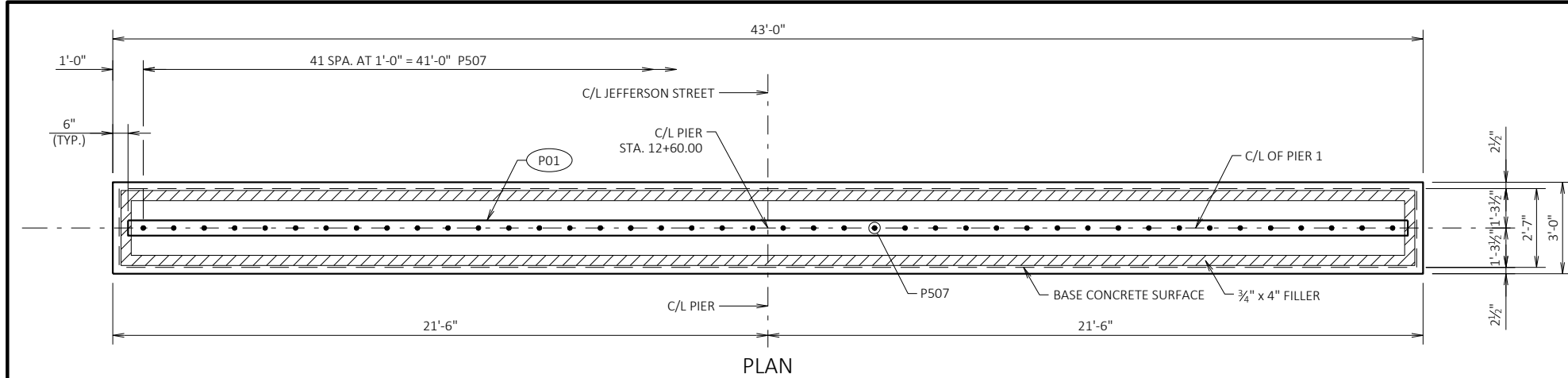
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-51-154

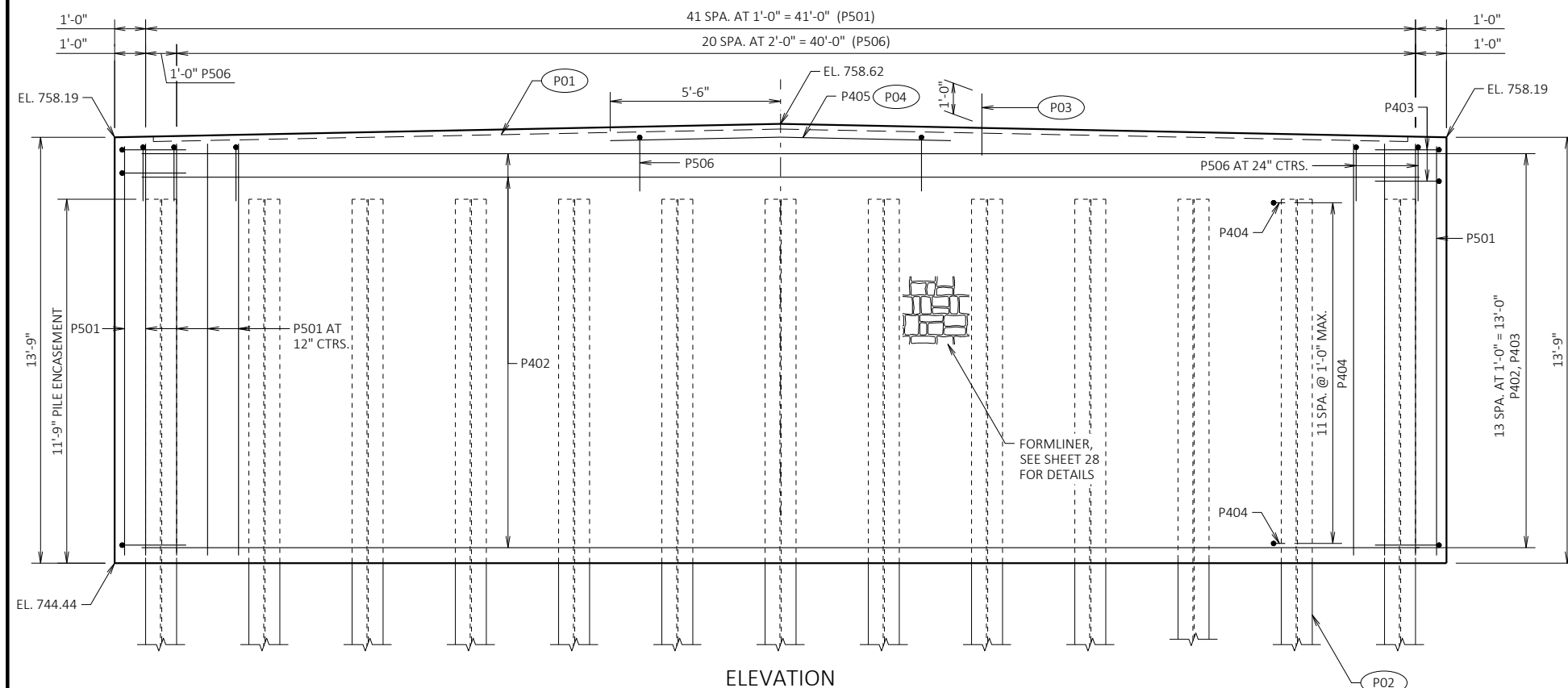
DRAWN BY PKF PLANS CK'D. BH

PIER 1

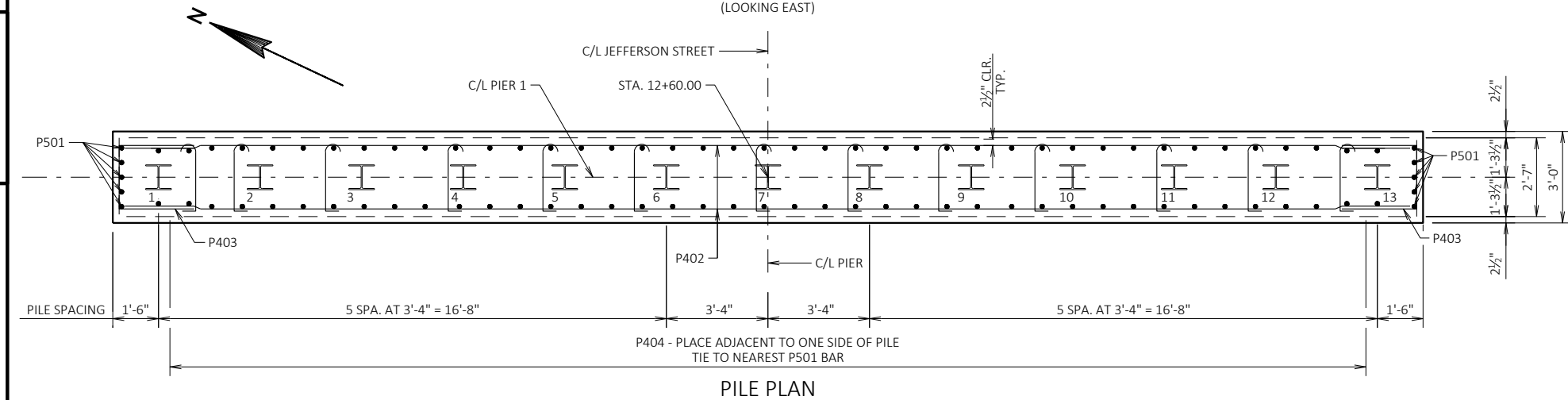
SHEET 12 OF 28



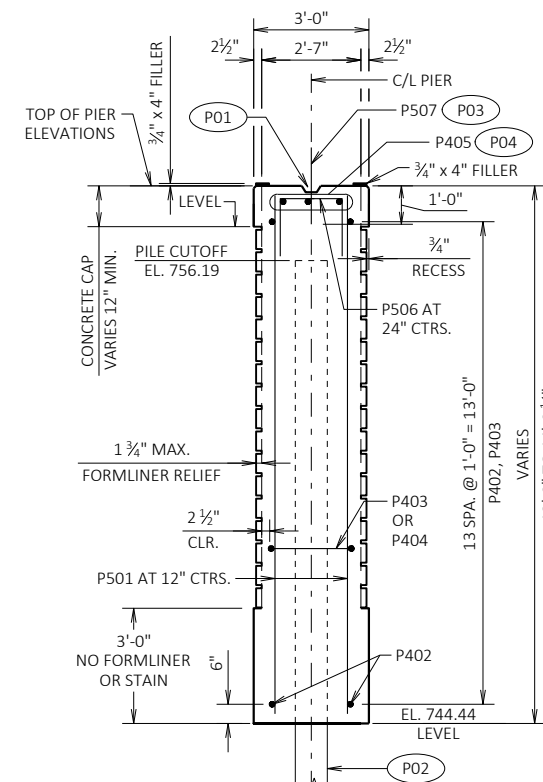
PLAN



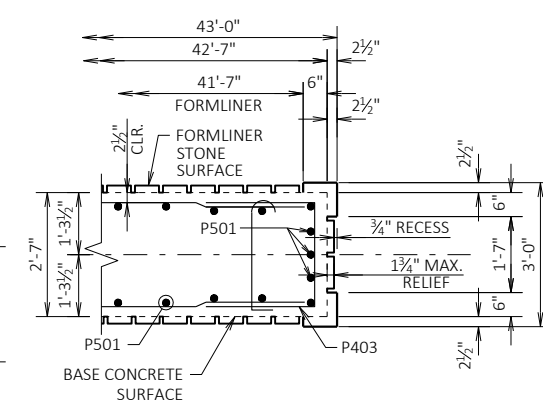
ELEVATION
(LOOKING EAST)



PILE PLAN



TYPICAL PIER PLAN WITH STONE FORMLINER



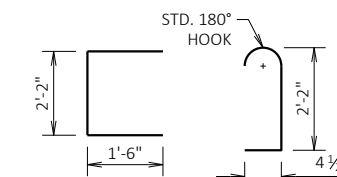
BILL OF BARS - PIER 2

BAR MARK	NO. REQ'D	LENGTH	BENT	BAR SERIES	LOCATION	
NON-COATED BARS						TOTAL WEIGHT = 2,620 LBS
Q501	94	13'-1"			SHAFT	VERT.
Q402	28	42'-2"			SHAFT	HORIZ.
Q403	28	5'-0"	X		SHAFT - ENDS	HORIZ.
Q404	156	3'-0"	X		SHAFT - TIE	HORIZ.
Q405	3	11'-0"			SHAFT - TOP	HORIZ.
Q506	22	4'-10"	X		SHAFT - TOP	VERT.
COATED BARS						TOTAL WEIGHT = 90 LBS
Q507	42	2'-0"			SHAFT - TOP	VERT.

THE FIRST DIGIT OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE.
DIMENSIONS IN BENDING DETAILS ARE OUT-TO-OUT OF BAR

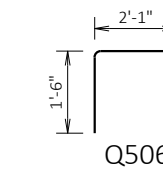
NOTES

COFFERDAM REQUIRED. CONCRETE POURED UNDER WATER WILL BE ALLOWED AND SHALL BE DONE IN ACCORDANCE WITH THE STANDARD SPEC. 502.3.5.3. CONCRETE POURED UNDER WATER SHALL NOT EXCEED 10.0 FEET IN DEPTH, UNLESS APPROVED OTHERWISE.



Q403

Q404



Q506

LEGEND

- P01 2" x 6" BEVELED KEYWAY.
- P02 PIERS SUPPORTED ON PILING STEEL HP 12-INCH x 53 LB PREBORED A MINIMUM OF 3 FEET INTO BEDROCK. ESTIMATED 30' LONG AT THE PIERS. PILES PLACED IN PREBORED HOLES BORED INTO ROCK DO NOT REQUIRE DRIVING. PILING SHALL BE FIRMLY SEATED AFTER PLACEMENT.
- P03 Q507 BARS MAY BE PLACED AFTER PIER IS POURED BUT BEFORE CONC. HAS SET. EMBED BARS 1'-0".
- P04 FIELD BEND BARS TO FOLLOW TOP SLOPE.

NO.	DATE	REVISION	BY

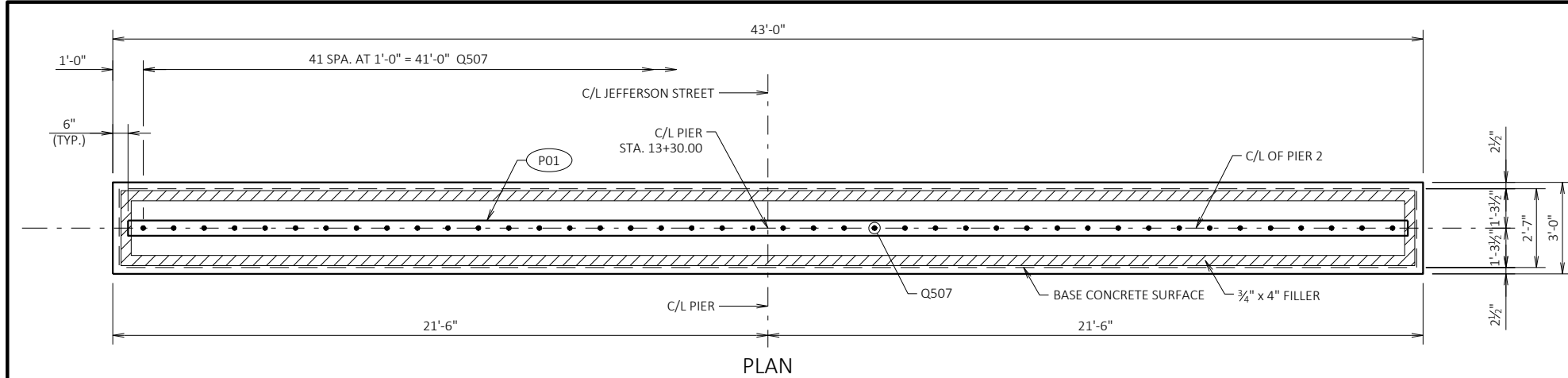
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-51-154

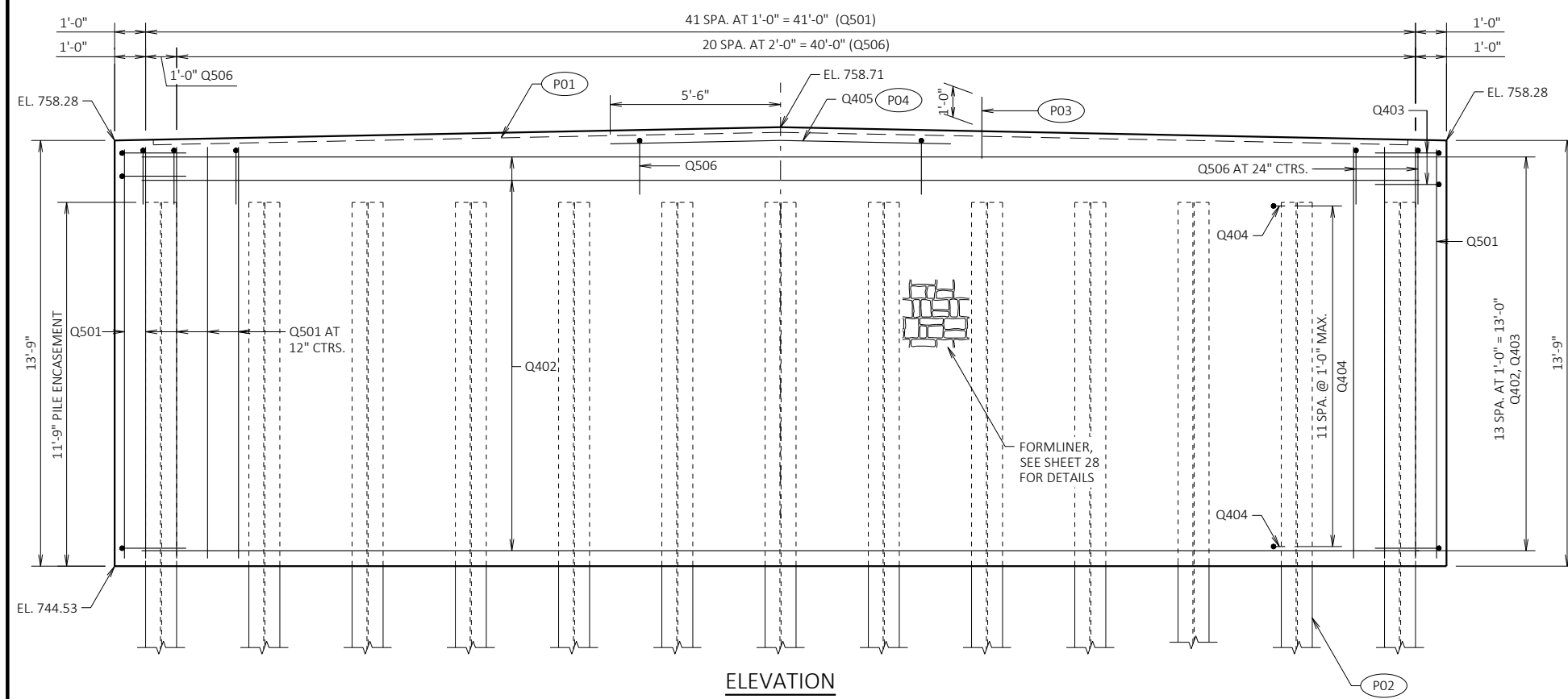
DRAWN BY PKF PLANS CK'D. BH

PIER 2

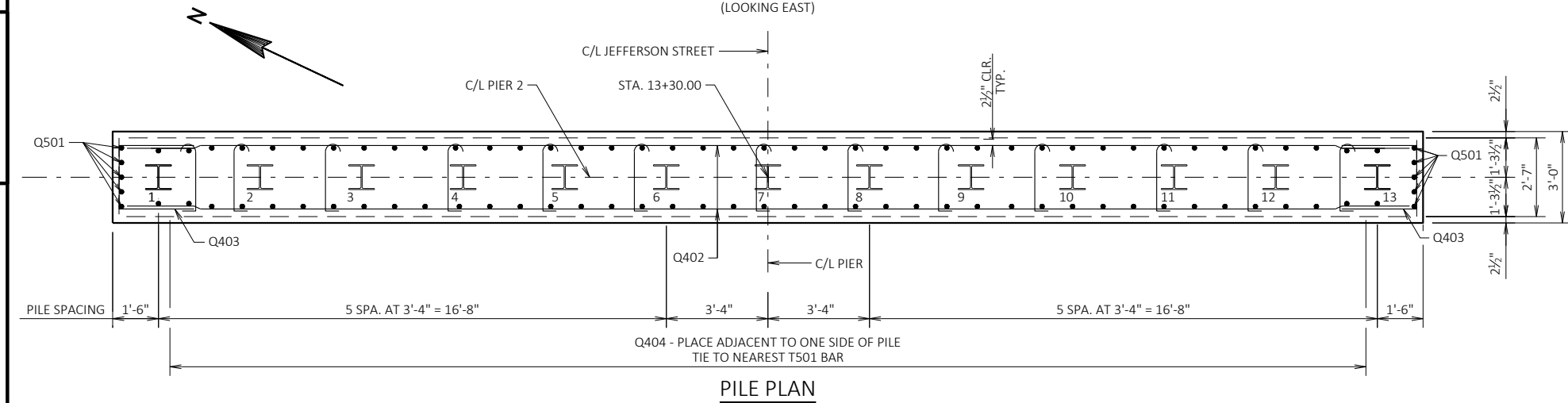
SHEET 13 OF 28



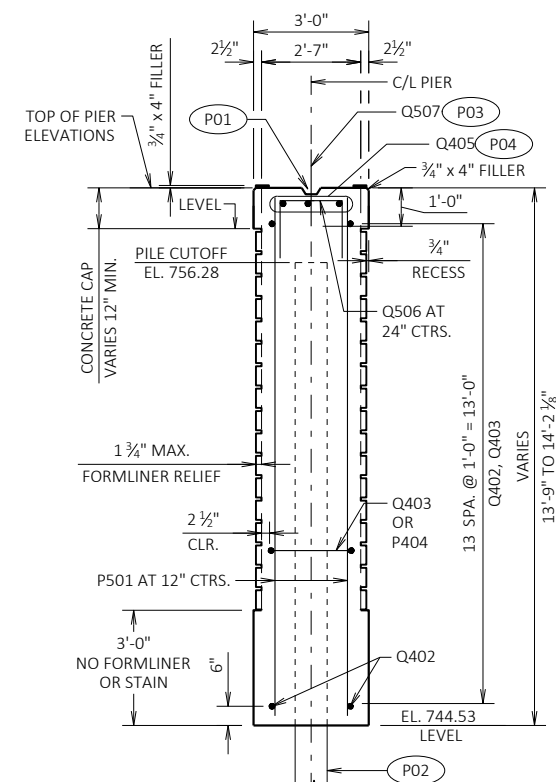
PLAN



ELEVATION
(LOOKING EAST)

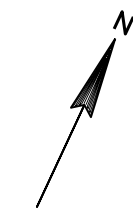


PILE PLAN



TYPICAL PIER PLAN WITH STONE FORMLINER





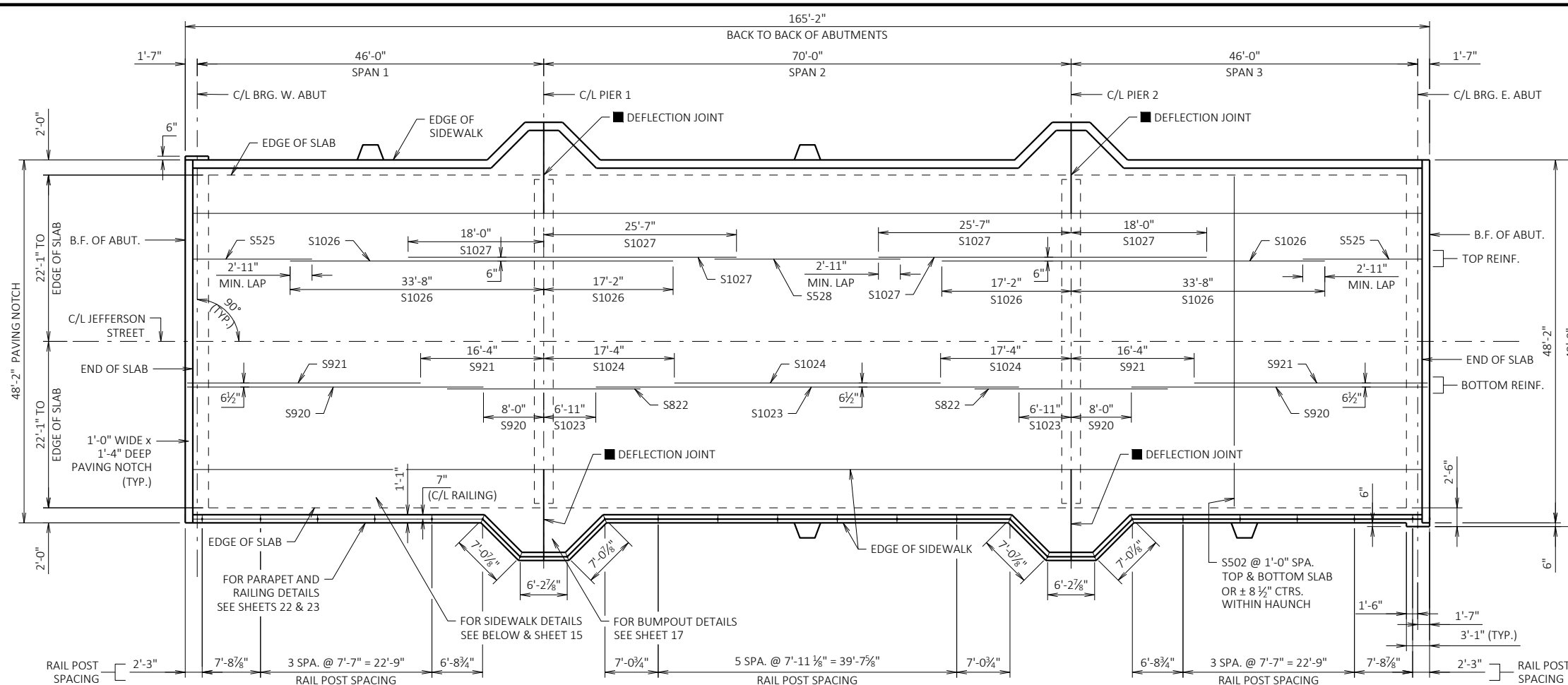
NOTES

TOP TRANSVERSE BARS IN SLAB SHALL BE SUPPORTED BY INDIVIDUAL BAR CHAIRS AT APPROXIMATELY 3'-0" CENTERS EACH WAY. BOTTOM LONGITUDINAL BARS SHALL BE SUPPORTED BY CONTINUOUS BAR CHAIRS AT APPROXIMATELY 4'-0" CENTERS.

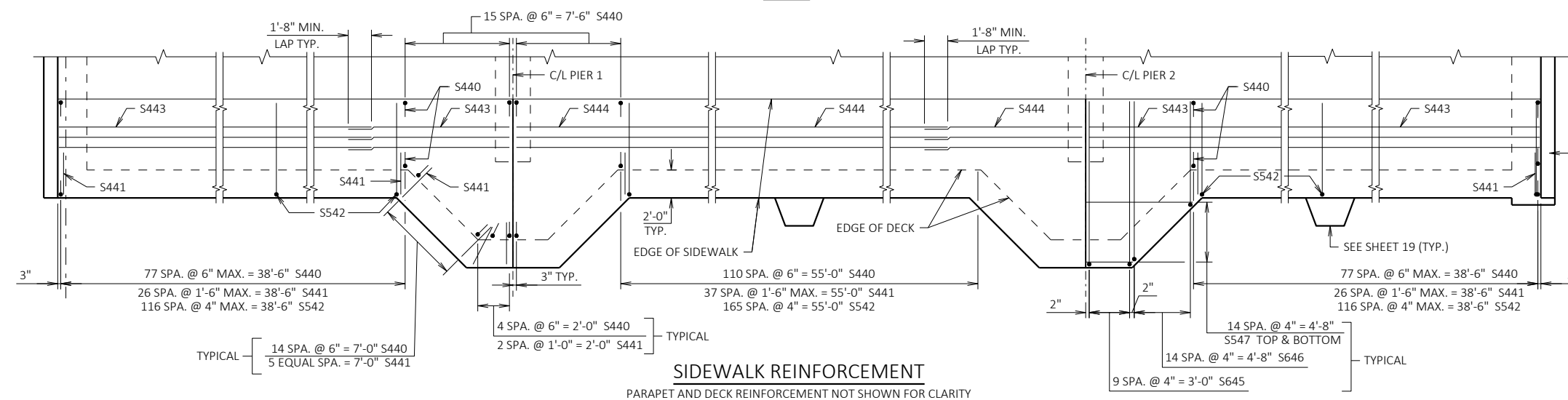
ALL SLAB THICKNESS DIMENSIONS ARE MINIMUM. ANY TOLERANCES NECESSARY TO CORRECT CONSTRUCTION DISCREPANCIES ARE TO BE PLUS (+).

PARAPETS AND SIDEWALKS PLACED ON TOP OF THE SLAB SHALL BE POURED AFTER FALSEWORK HAS BEEN RELEASED.

■ DEFLECTION JOINT IN SIDEWALK AND PARAPET AT C/L PIER TYP. SEE DETAIL, SHEET 22.

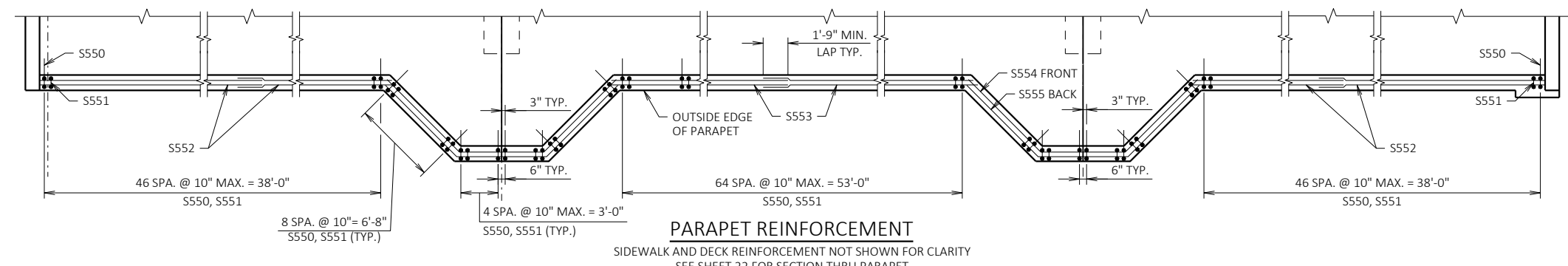


PLAN



SIDEWALK REINFORCEMENT

PARAPET AND DECK REINFORCEMENT NOT SHOWN FOR CLARITY

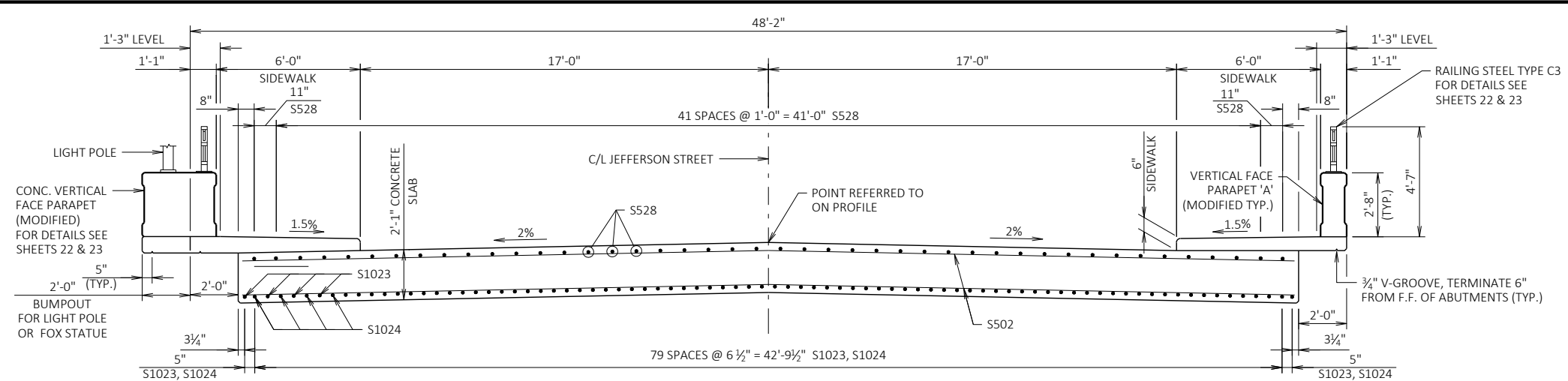


PARAPET REINFORCEMENT

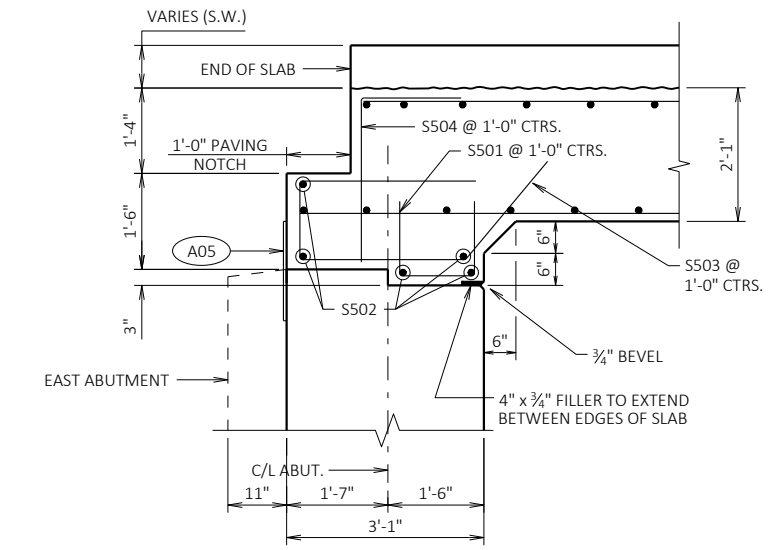
SIDEWALK AND DECK REINFORCEMENT NOT SHOWN FOR CLARITY SEE SHEET 22 FOR SECTION THRU PARAPET

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-51-154			
DRAWN BY PKF		PLANS CK'D. BH	
SUPERSTRUCTURE PLAN			SHEET 14 OF 28

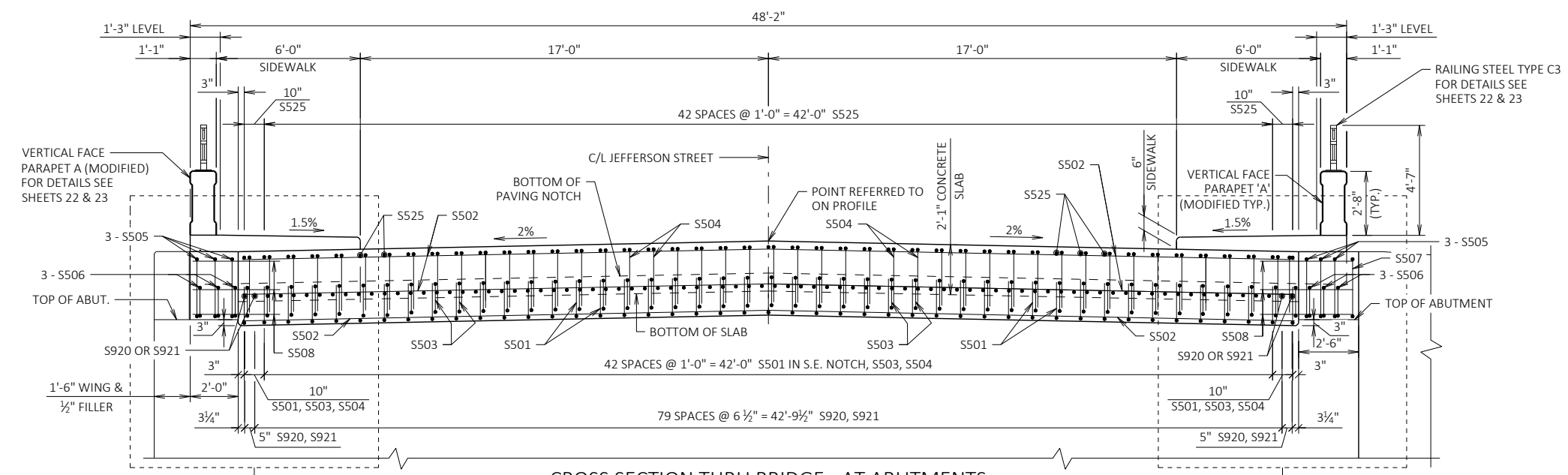




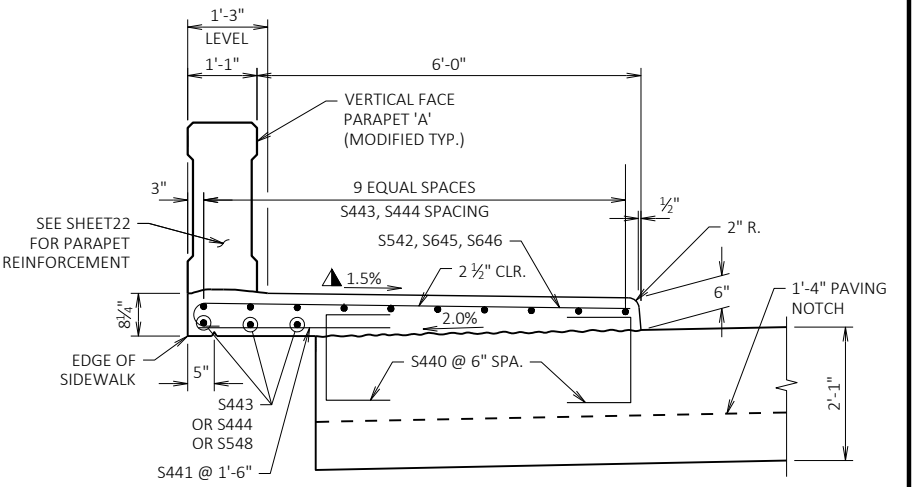
CROSS SECTION THRU BRIDGE - IN SPAN 2
(LOOKING EAST)



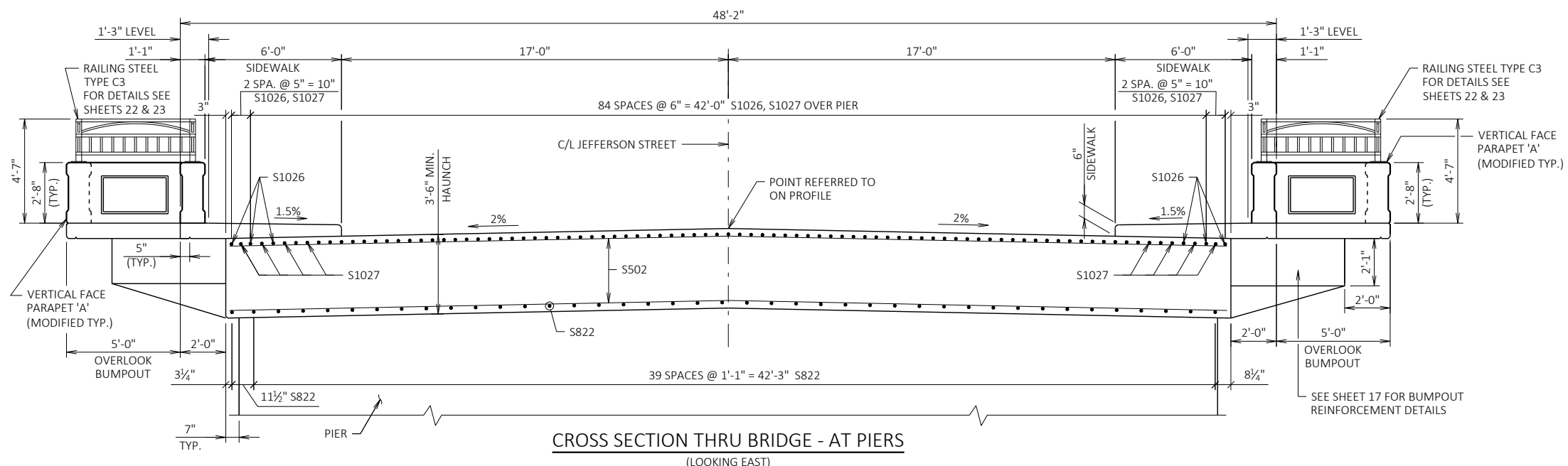
SECTION THRU SIDEWALK & SLAB
(PARAPET & S.W. STEEL NOT SHOWN FOR CLARITY)



CROSS SECTION THRU BRIDGE - AT ABUTMENTS
(LOOKING EAST - E. ABUT. SHOWN, W. SIMILAR)



SECTION THRU SIDEWALK



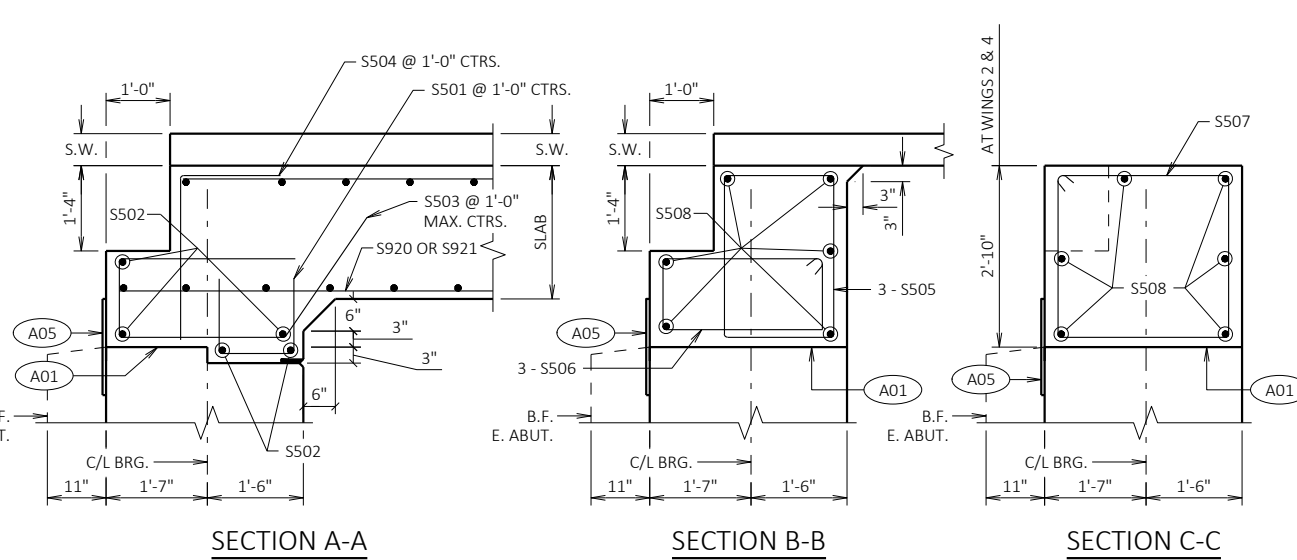
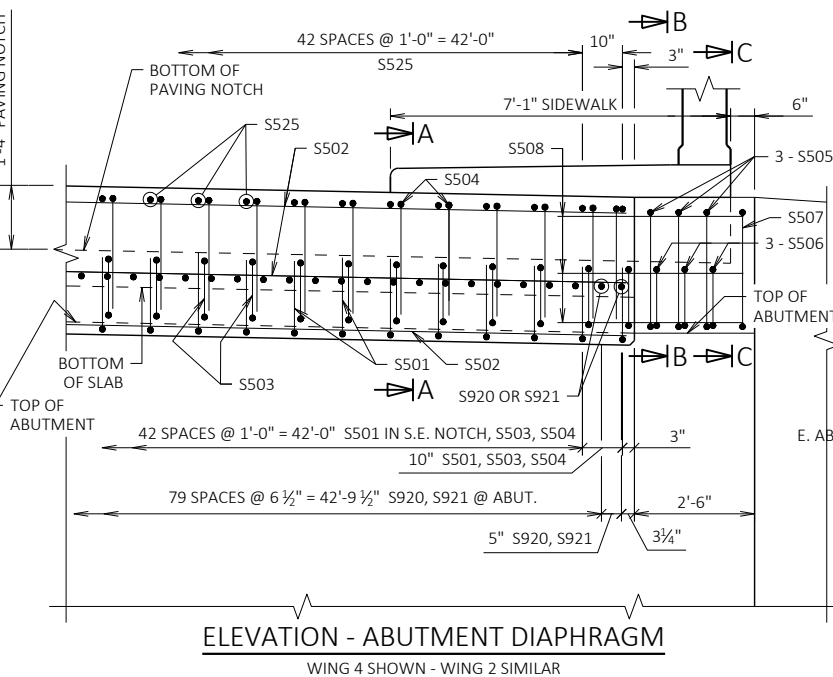
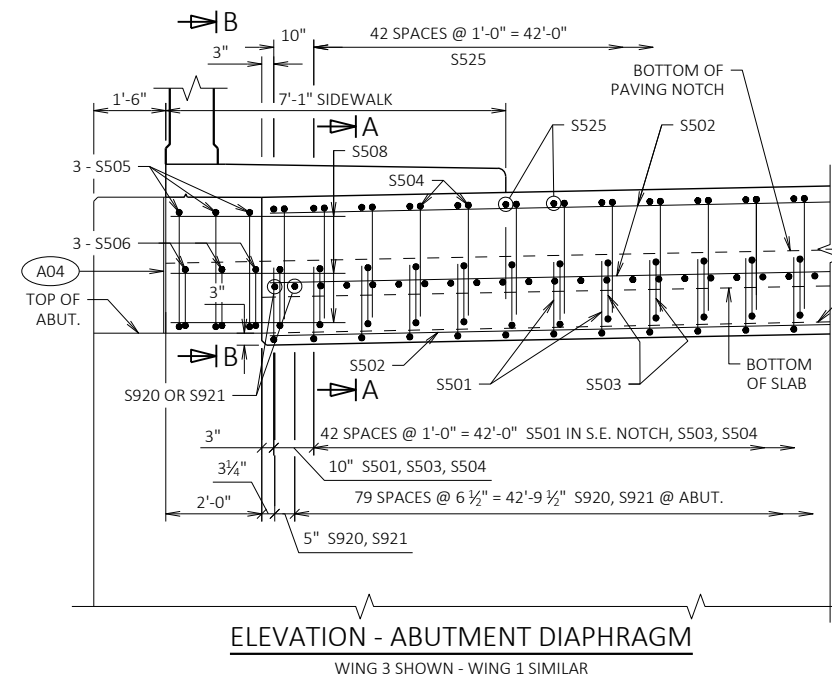
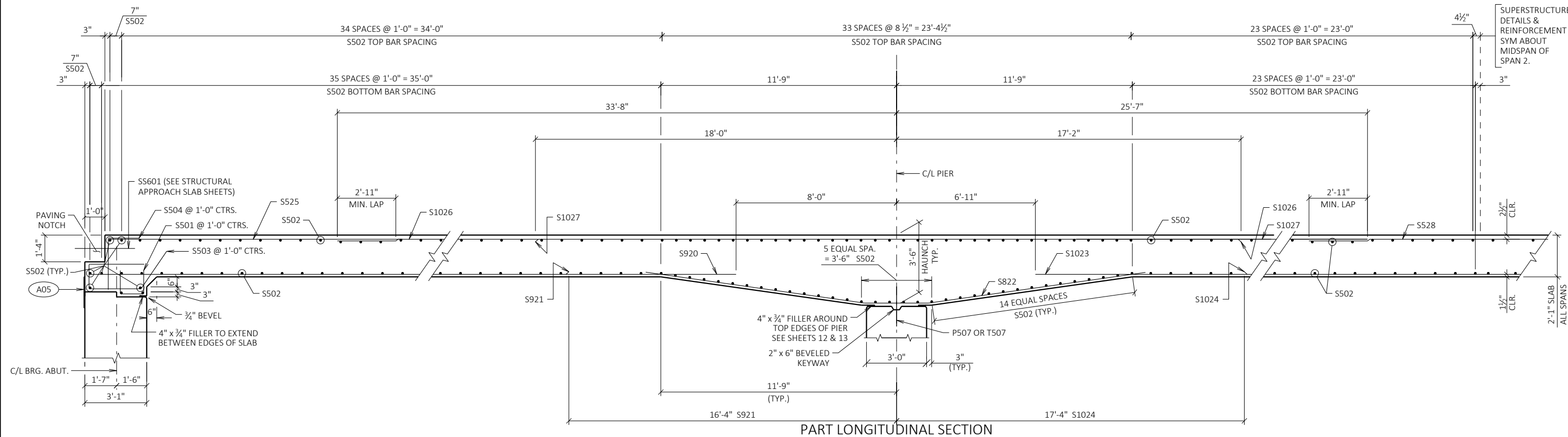
CROSS SECTION THRU BRIDGE - AT PIERS
(LOOKING EAST)

LEGEND

- A05 18" RUBBERIZED MEMBRANE WATERPROOFING. SEAL ALL HORIZONTAL AND VERTICAL JOINTS ON BACKFACE.
- ▲ ± 0.5% CONSTRUCTION TOLERANCE IN CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM ENGINEER.

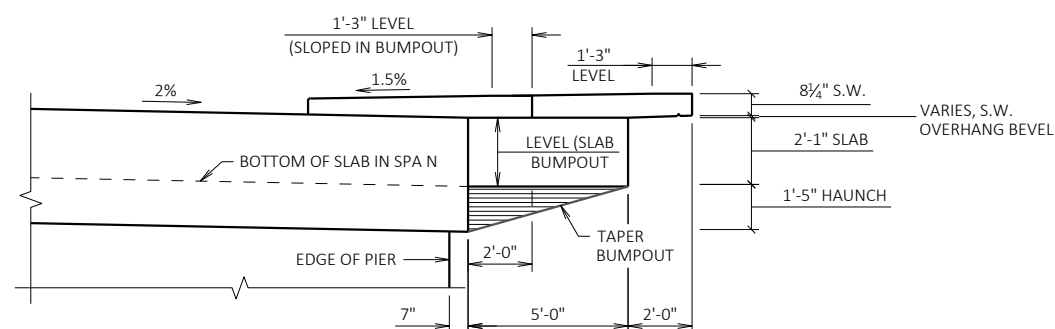
NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-51-154			
DRAWN BY PKF		PLANS CK'D. BH	
SUPERSTRUCTURE SECTIONS			SHEET 15 OF 28



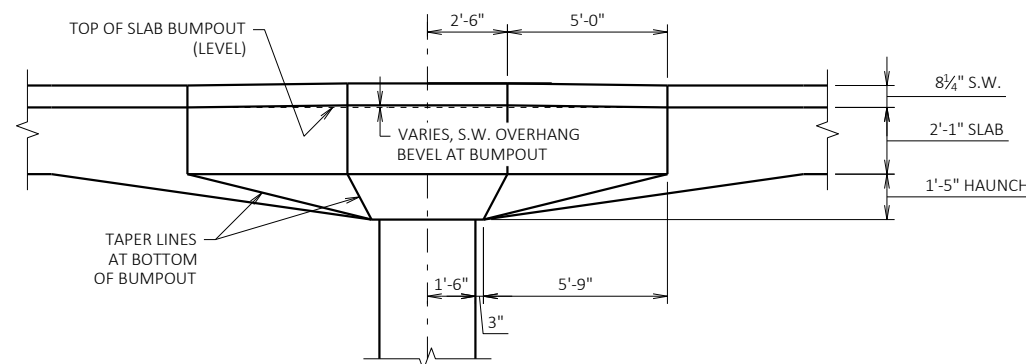


- LEGEND**
- A01 STEEL TROWEL TOP SURFACE OF ABUTMENT. PLACE MULTIPLE LAYERS OF POLYETHYLENE SHEETS OVER ENTIRE ABUTMENT TOP BEFORE PLACING SUPERSTRUCTURE. TOTAL THICKNESS OF SHEETS SHALL BE AT LEAST 0.03".
 - A04 1/2" FILLER: SEAL ALL EXPOSED HORIZONTAL AND VERTICAL SURFACES OF 1/2" FILLER WITH NON-STAINING, NON-BITUMINOUS JOINT SEALER. (1" DEEP AND HOLD 1/8" BELOW SURFACE OF CONCRETE). COLOR TO MATCH ADJACENT SURFACE.
 - A05 18" RUBBERIZED MEMBRANE WATERPROOFING. SEAL ALL HORIZONTAL AND VERTICAL JOINTS ON BACKFACE.

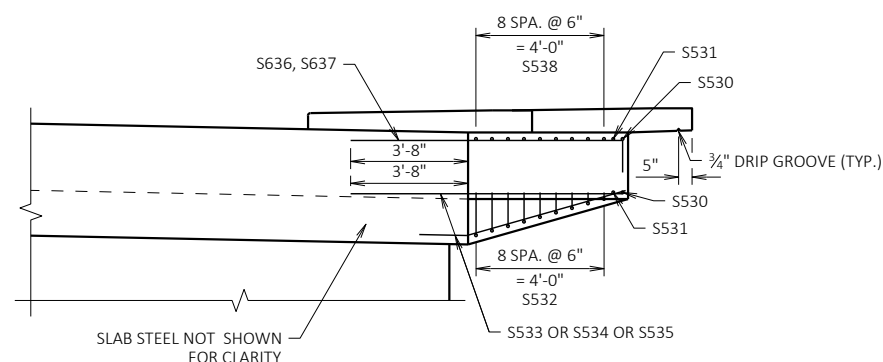
NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-51-154			
DRAWN BY PKF		PLANS CK'D. BH	
SUPERSTRUCTURE & SIDEWALK SECTIONS			SHEET 16 OF 28



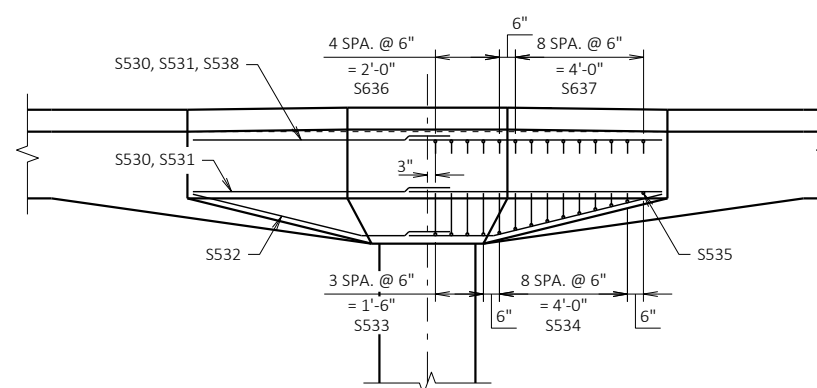
CROSS-SECTION OF BUMPOUT AT PIER
(PARAPET NOT SHOWN FOR CLARITY)



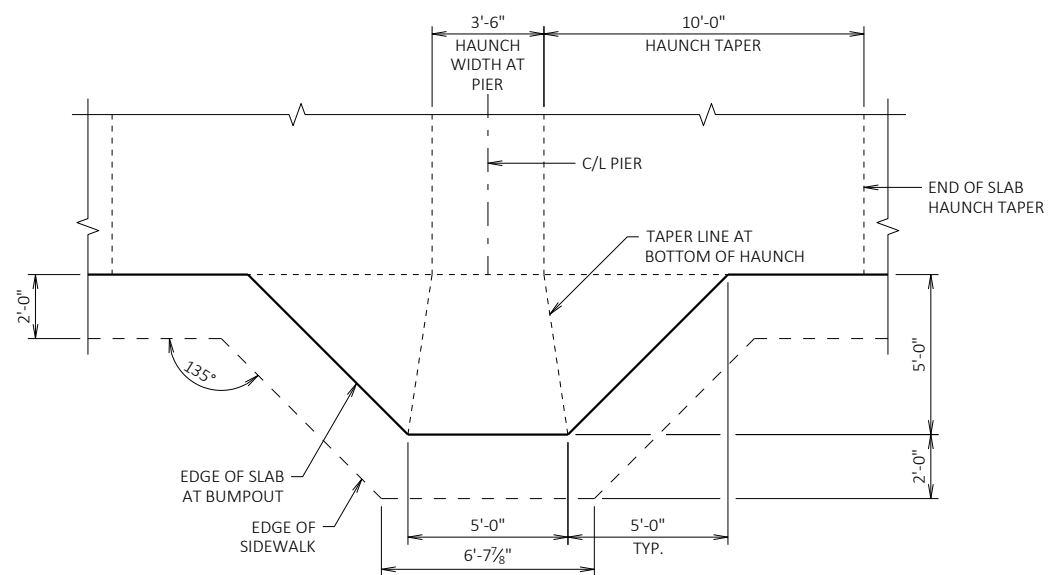
ELEVATION OF BUMPOUT AT PIER
(PARAPET NOT SHOWN FOR CLARITY)



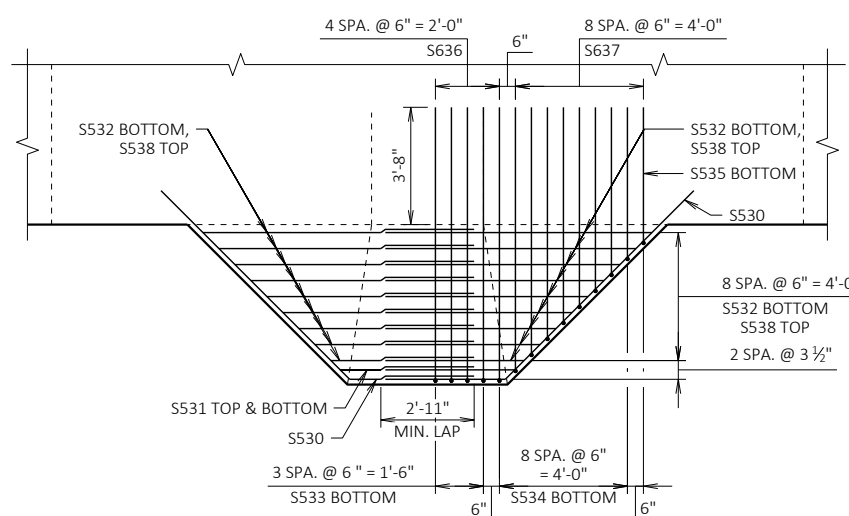
CROSS-SECTION OF BUMPOUT AT PIER - REBAR DETAILS
(PARAPET NOT SHOWN FOR CLARITY)



ELEVATION OF BUMPOUT AT PIER - REBAR DETAILS
(PARAPET NOT SHOWN FOR CLARITY)



PLAN OF BUMPOUT AT PIER



PLAN OF BUMPOUT AT PIER - REBAR DETAILS

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-51-154			
DRAWN BY PKF		PLANS CK'D. BH	
SUPERSTRUCTURE BUMPOUT DETAILS			SHEET 17 OF 28

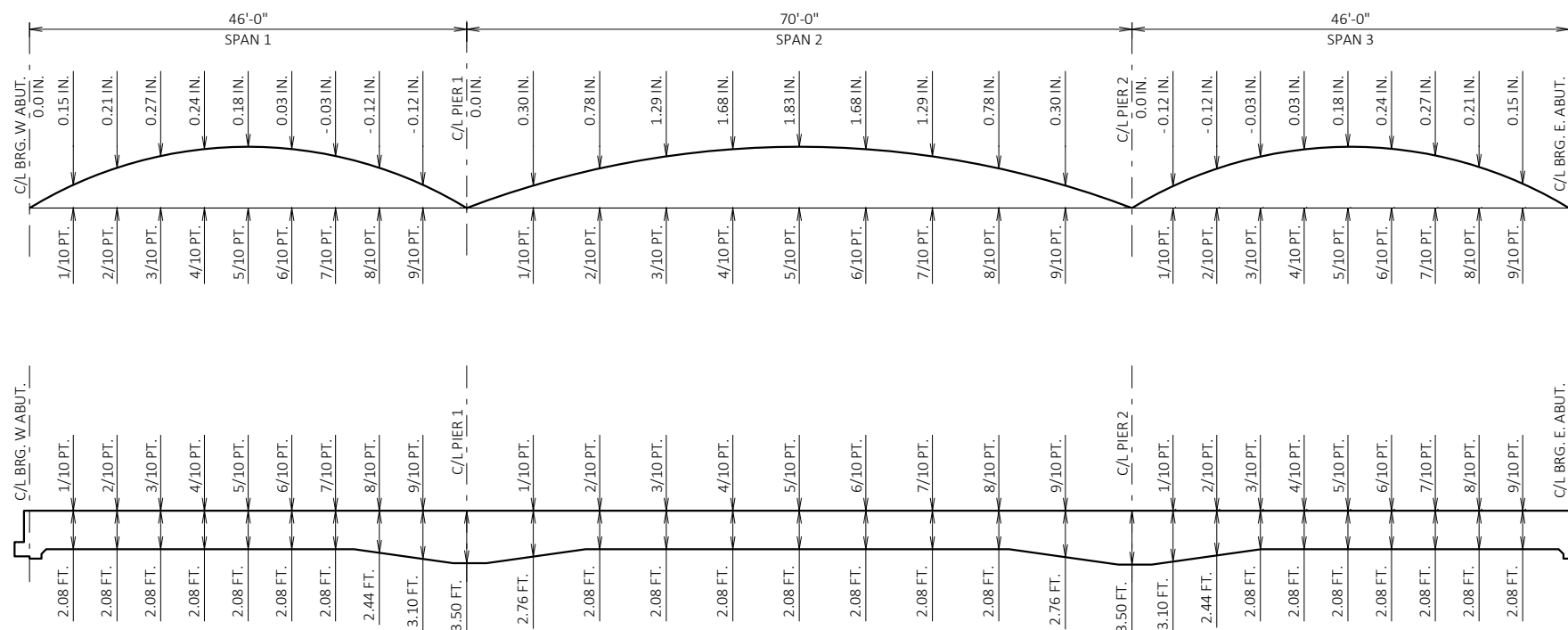


8

8

TOP OF SLAB ELEVATIONS

LOCATION	C/L BRG. W. ABUT.	0.1 PT.	0.2 PT.	0.3 PT.	0.4 PT.	0.5 PT.	0.6 PT.	0.7 PT.	0.8 PT.	0.9 PT.	C/L PIER 1	0.1 PT.	0.2 PT.	0.3 PT.	0.4 PT.	0.5 PT.	0.6 PT.	0.7 PT.	0.8 PT.	0.9 PT.	C/L PIER 2	0.1 PT.	0.2 PT.	0.3 PT.	0.4 PT.	0.5 PT.	0.6 PT.	0.7 PT.	0.8 PT.	0.9 PT.	C/L BRG. E. ABUT.
NORTH EDGE OF DECK	761.06	761.15	761.23	761.31	761.39	761.46	761.52	761.58	761.64	761.69	761.74	761.80	761.85	761.88	761.91	761.92	761.93	761.92	761.90	761.87	761.83	761.80	761.76	761.72	761.67	761.62	761.56	761.50	761.43	761.36	761.28
C/L JEFFERSON STREET	761.50	761.59	761.68	761.76	761.83	761.90	761.97	762.03	762.08	762.13	762.18	762.24	762.29	762.33	762.35	762.37	762.37	762.36	762.34	762.31	762.27	762.24	762.20	762.16	762.11	762.06	762.00	761.94	761.87	761.80	761.72
SOUTH EDGE OF DECK	761.06	761.15	761.23	761.31	761.39	761.46	761.52	761.58	761.64	761.69	761.74	761.80	761.85	761.88	761.91	761.92	761.93	761.92	761.90	761.87	761.83	761.80	761.76	761.72	761.67	761.62	761.56	761.50	761.43	761.36	761.28



CAMBER AND SLAB THICKNESS DIAGRAM

CAMBER IS BASED ON 3 TIMES DEAD LOAD DEFLECTIONS. CAMBER SPANS AS SHOWN TO PROVIDE FOR DEADLOAD DEFLECTION & FUTURE CREEP. CAMBER DOES NOT INCLUDE ALLOWANCE FOR FORM SETTLEMENT. PARAPETS AND SIDEWALKS SHOWN ABOVE THE HORIZ. CONST. JT. SHALL BE POURED AFTER FALSEWORK HAS BEEN RELEASED.

TO DETERMINE FALSEWORK ELEVATION AT EDGE OF SLAB OR CROWN FOLLOW THIS PROCEDURE:

- TOP OF SLAB ELEVATION AT FINAL GRADE
- LESS (-) SLAB THICKNESS
- PLUS (+) CAMBER
- PLUS (+) FORM SETTLEMENT/DEFLECTION DUE TO PLACEMENT OF SLAB CONCRETE (TO BE COMPUTED BY THE CONTRACTOR)
- EQUALS TOP OF SLAB FALSEWORK ELEVATION.

SURVEY TOP OF SLAB ELEVATIONS

	C/L BRG. W. ABUT.	5/10 PT.	C/L PIER 1	5/10 PT.	C/L PIER 2	5/10 PT.	C/L BRG. E. ABUT.
NORTH GUTTERLINE							
C/L JEFFERSON ST./CROWN POINT							
SOUTH GUTTERLINE							

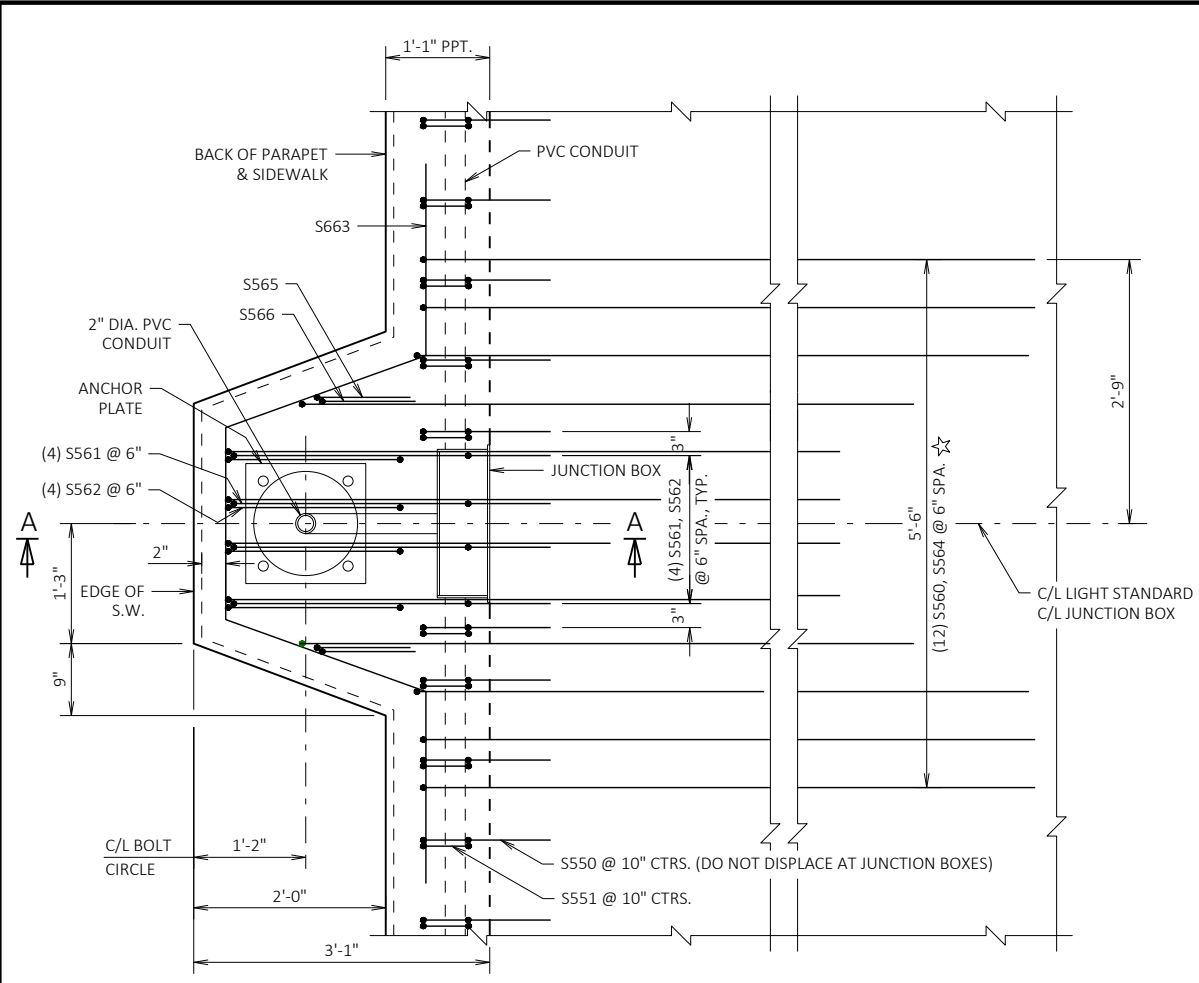
PRIOR TO RELEASING SLAB FALSEWORK, TAKE TOP OF SLAB ELEVATIONS AT THE C/L OF ABUTMENTS, THE C/L OF PIERS, AND AT 5/10 PTS. TO VERIFY CAMBER. TAKE ELEVATIONS ALONG GUTTERLINES AND CROWN. RECORD THE ELEVATIONS IN THE ABOVE TABLE FOR THE "AS BUILT" PLANS.

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-51-154			
DRAWN BY		PKF	PLANS CK'D. BH
SUPERSTRUCTURE DETAILS			SHEET 18 OF 28



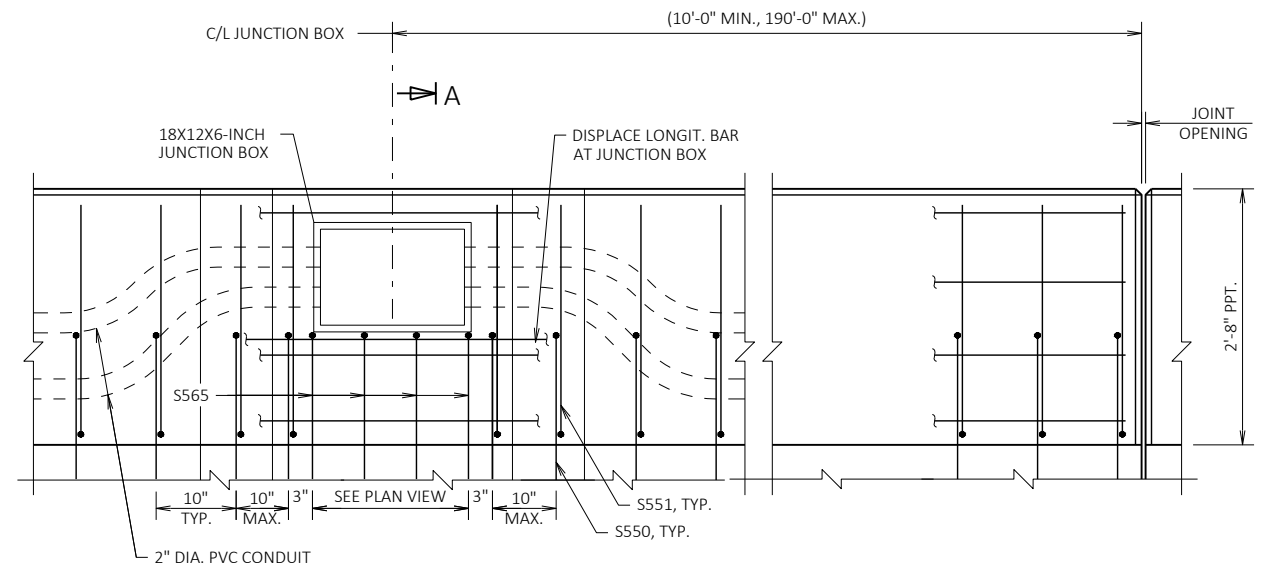
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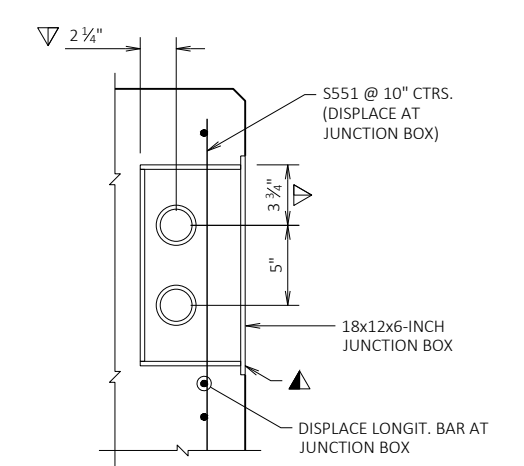
PLAN AT LIGHT STANDARD

(SIDEWALK STEEL NOT SHOWN FOR CLARITY)



INSIDE ELEVATION AT JUNCTION BOX

(DECK STEEL NOT SHOWN FOR CLARITY)



JUNCTION BOX DETAIL

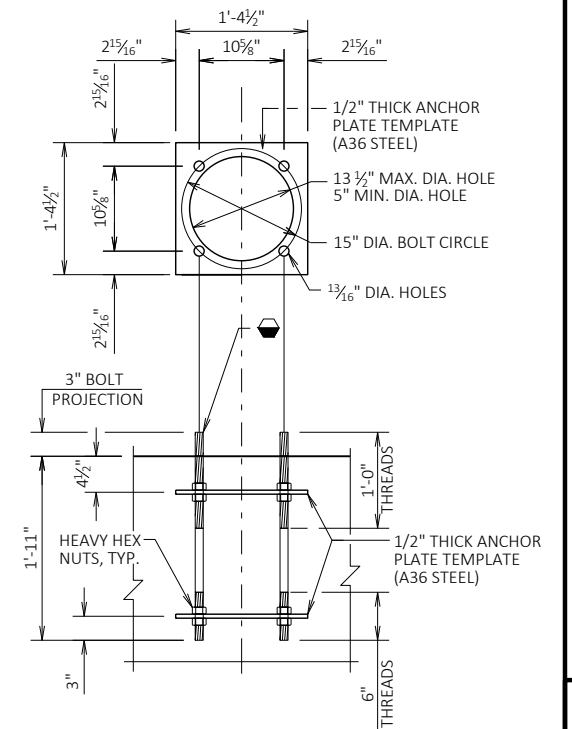
NOTES

- BID ITEM FOR ANCHOR ROD ASSEMBLY SHALL BE "ANCHOR ASSEMBLIES POLES ON STRUCTURES", EACH.
- CONDUIT SHALL BE EMBEDDED 2" CLEAR.
- USE 2" DIA. RIGID NONMETALLIC CONDUIT (PVC) UNLESS NOTED OTHERWISE.
- CONDUIT FITTINGS, CONDUIT BENDS, AND ADAPTER FITTINGS INCIDENTAL TO CONDUIT WORK.
- PROVIDE JUNCTION BOXES FROM THE APPROVED PRODUCTS LIST.
- SEE "CONDUIT DETAILS" SHEET FOR ADDITIONAL INFORMATION.
- SEE SHEET 22 FOR "SECTION THRU PARAPET ON BRIDGE".
- LIGHT STANDARD DETAILS SHOWN. REINFORCEMENT FOR FOX STATUE BUMPOUT SIMILAR.

LEGEND

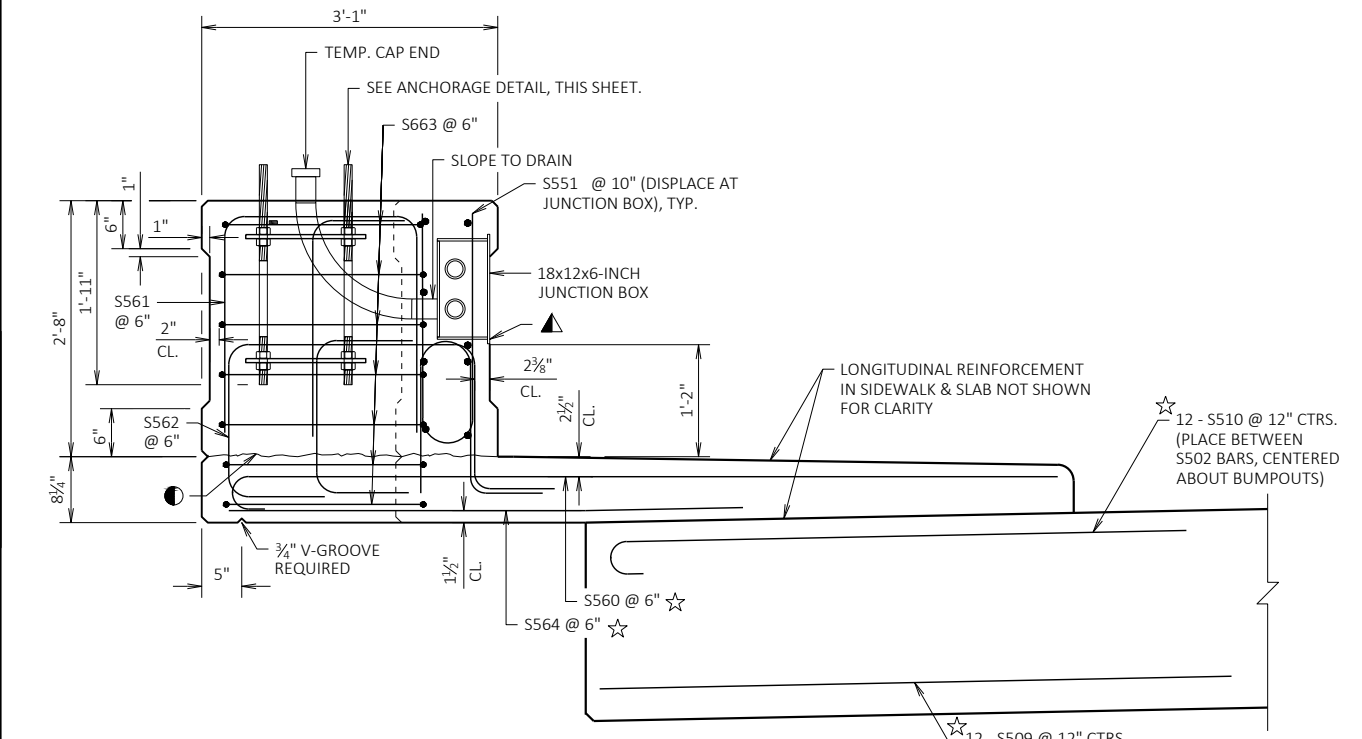
- CONSTRUCTION JOINT, STRIKE OFF AS SHOWN.
- ▲ CUT OUT ± 1" OF GASKET AT BOTTOM OF JUNCTION BOX COVER TO ALLOW FOR DRAINAGE.
- ▽ LOCATION OF CONDUIT IS MEASURED FROM OUTSIDE EDGE OF JUNCTION BOX.
- ☆ THESE BARS ARE IN ADDITION TO STANDARD TRANSVERSE BARS IN SIDEWALK OR SLAB.
- 3/4" DIA. ANCHOR RODS ASTM F1554 GR 55, HEAVY HEX NUTS ASTM A563, AND WASHERS ASTM F436. ANCHOR ASSEMBLIES SHALL BE GALVANIZED PER SECTION 531 OF THE STANDARD SPECIFICATION. PROVIDE (2) WASHERS AND (4) NUTS PER ANCHOR ROD. CONFIRM ANCHOR ROD ASSEMBLY DETAILS WITH SUPPLIER OF LIGHT POST PRIOR TO INSTALLATION. ALL NUTS, WASHERS, ETC. REQUIRED FOR CONNECTION OF LIGHT POST SHALL BE PROVIDED BY LIGHT POST SUPPLIER.

PVC = POLYVINYL CHLORIDE (RIGID NONMETALLIC) CONDUIT



ANCHOR ROD ASSEMBLY DETAIL

(2 REQ'D.)
(4) - ANCHOR RODS REQUIRED PER ASSEMBLY



SECTION A-A

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-51-154			
DRAWN BY		PKE	PLANS CK'D. BH
LIGHT STANDARD AND JUNCTION BOX DETAILS			SHEET 19 OF 28



BILL OF BARS - SUPERSTRUCTURE

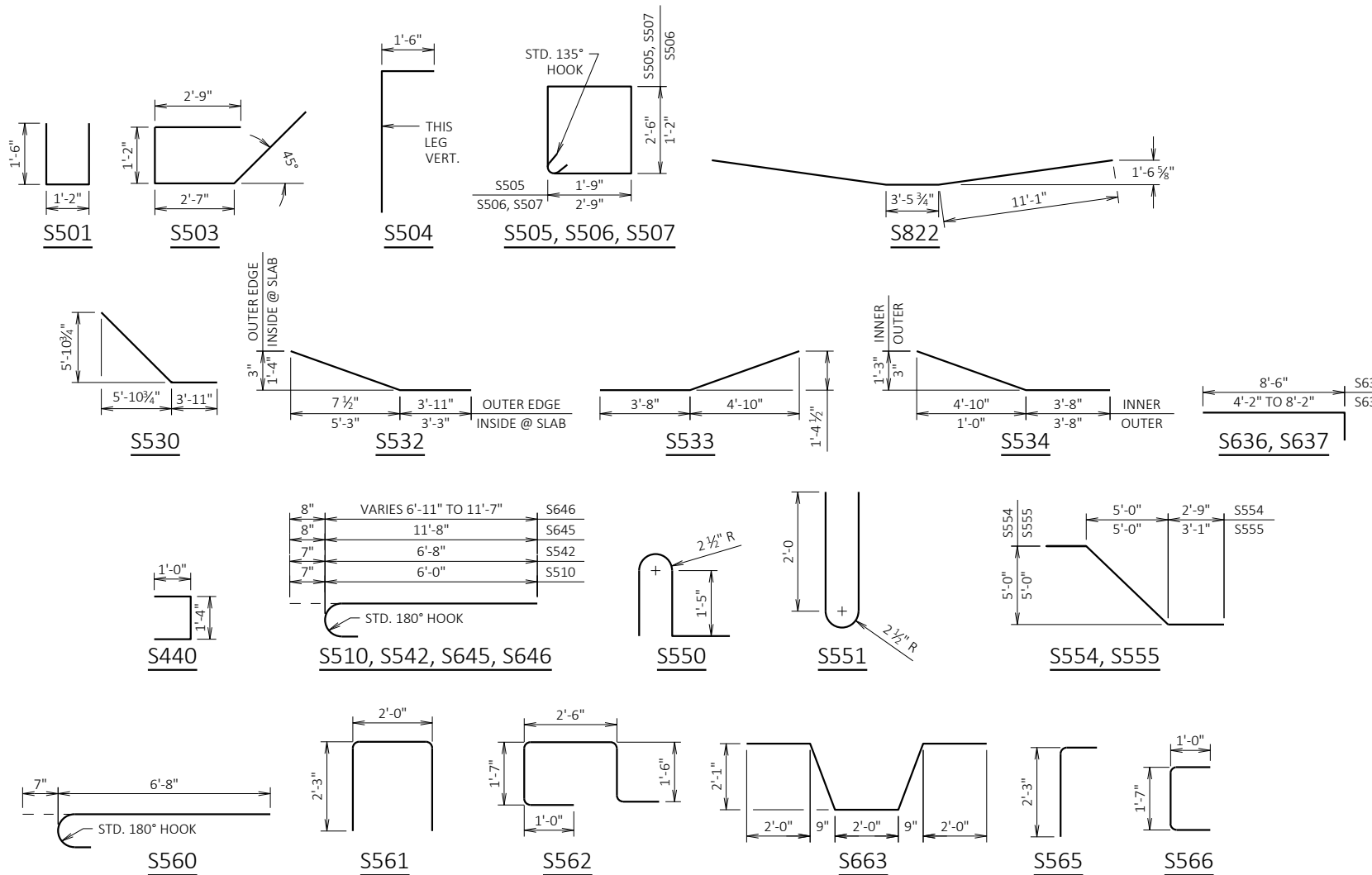
BAR MARK	NO. REQ'D	LENGTH	BENT	BAR SERIES	LOCATION
COATED BARS					
					TOTAL WEIGHT = 130,310 LBS
S501	90	3'-11"	X		SLAB AT ABUTMENT NOTCH VERT.
S502	384	43'-10"			SLAB - TRANSVERSE BARS TRANS.
S503	90	8'-4"	X		SLAB AT ABUTMENTS VERT.
S504	90	3'-10"	X		SLAB AT ABUTMENTS VERT.
S505	12	9'-2"	X		SLAB AT ABUTMENT ENDS VERT.
S506	12	8'-6"	X		SLAB AT ABUTMENT ENDS VERT.
S507	2	11'-2"	X		SLAB AT ABUTMENT ENDS NEAR WINGS 2 & 4 VERT.
S508	24	4'-0"			SLAB AT ABUTMENT ENDS HORIZ.
S509	48	6'-0"			SLAB AT LIGHT STANDARD OR STATUE BUMPOUT TRANS.
S510	48	6'-7"	X		SLAB AT LIGHT STANDARD OR STATUE BUMPOUT TRANS.
S920	82	39'-5"			SLAB - BOTTOM, SPANS 1 & 3 LONG.
S921	82	31'-1"			SLAB - BOTTOM, SPANS 1 & 3 LONG.
S822	82	25'-8"	X		SLAB - BOTTOM, OVER PIERS LONG.
S1023	41	56'-2"			SLAB - BOTTOM, SPAN 2 LONG.
S1024	41	35'-4"			SLAB - BOTTOM, SPAN 2 LONG.
S525	90	15'-9"			SLAB - TOP, SPANS 1 & 3 LONG.
S1026	90	50'-10"			SLAB - TOP, OVER PIERS LONG.
S1027	88	43'-7"			SLAB - TOP, OVER PIERS LONG.
S528	88	24'-9"			SLAB - TOP, SPAN 2 LONG.
S530	16	12'-3"	X		SLAB BUMPOUT AT EDGE LONG.
S531	8	5'-5"			SLAB BUMPOUT LONG.
S532	72	6'-8"	X	X	SLAB BUMPOUT - BOTTOM LONG.
S533	32	8'-9"	X		SLAB BUMPOUT - BOTTOM TRANS.
S534	72	6'-8"	X	X	SLAB BUMPOUT - BOTTOM TRANS.
S535	8	4'-2"			SLAB BUMPOUT - BOTTOM TRANS.
S636	40	9'-4"	X		SLAB BUMPOUT - TOP TRANS.
S637	72	7'-0"	X	X	SLAB BUMPOUT - TOP TRANS.
S538	72	6'-6"		X	SLAB BUMPOUT - TOP LONG.
S440	1356	3'-2"	X		SIDEWALK - TIE BARS VERT.
S441	256	3'-0"			SIDEWALK AT OVERHANG - BOTTOM TRANS.
S542	800	7'-3"	X		SIDEWALK AT TOP TRANS.
S443	104	24'-0"			SIDEWALK TOP & BOTTOM LONG.
S444	52	35'-9"			SIDEWALK TOP & BOTTOM LONG.
S645	80	12'-4"	X		SIDEWALK AT BUMPOUT - TOP TRANS.
S646	120	9'-11"	X	X	SIDEWALK AT BUMPOUT - TOP TRANS.
S547	240	5'-5"		X	SIDEWALK AT BUMPOUT - TOP & BOTTOM LONG.
S550	430	4'-4"	X		PARAPETS VERT.
S551	430	4'-9"	X		PARAPETS VERT.
S552	64	20'-3"			PARAPETS - SPANS 1 & 3 LONG.
S553	32	28'-0"			PARAPETS - SPAN 2 LONG.
S554	32	11'-11"	X		PARAPETS AT BUMPOUTS, F.F. LONG.
S555	32	12'-2"	X		PARAPETS AT BUMPOUTS, B.F. LONG.
S560	48	7'-3"	X		LIGHT STD. OR STATUE BUMPOUT - TOP AT S.W. TRANS.
S561	16	6'-3"	X		LIGHT STD. OR STATUE BUMPOUT VERT.
S562	16	7'-1"	X		LIGHT STD. OR STATUE BUMPOUT VERT.
S663	28	10'-0"	X		LIGHT STD. OR STATUE BUMPOUT HORIZ.
S564	48	6'-8"			LIGHT STD. OR STATUE BUMPOUT - BTM AT S.W. TRANS.
S565	8	3'-2"	X		LIGHT STD. OR STATUE BUMPOUT VERT.
S566	8	3'-4"	X		LIGHT STD. OR STATUE BUMPOUT VERT.

THE FIRST DIGIT OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE.
 DIMENSIONS IN BENDING DETAILS ARE OUT-TO-OUT OF BAR.
 LENGTH SHOWN FOR BAR SERIES IS AN AVERAGE. SEE BAR SERIES TABLE FOR ACTUAL LENGTHS.
 USE REINFORCEMENT DETAILS ON SHEET 19 FOR STATUE BUMPOUTS.

BAR SERIES TABLE

BAR MARK	NO. REQ'D.	LENGTH
S532	8 SERIES OF 9	4'-7" TO 8'-8"
S534	8 SERIES OF 9	4'-8" TO 8'-8"
S637	8 SERIES OF 9	5'-0" TO 9'-0"
S538	8 SERIES OF 9	4'-6" TO 8'-6"
S646	8 SERIES OF 15	7'-7" TO 12'-3"
S547	16 SERIES OF 15	3'-1" TO 7'-9"

BUNDLE & TAG EACH SERIES SEPARATELY

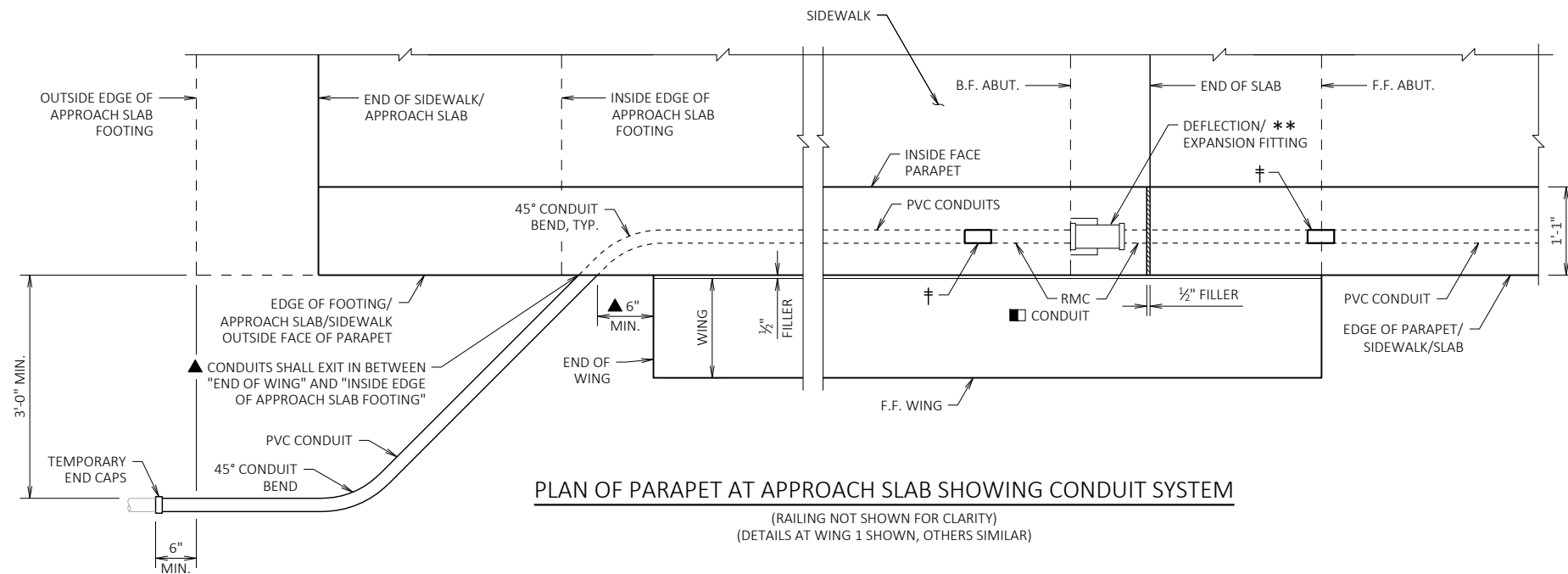


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NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-51-154			
DRAWN BY		PKF	PLANS CK'D. BH
SUPERSTRUCTURE REINFORCEMENT			SHEET 20 OF 28





PLAN OF PARAPET AT APPROACH SLAB SHOWING CONDUIT SYSTEM

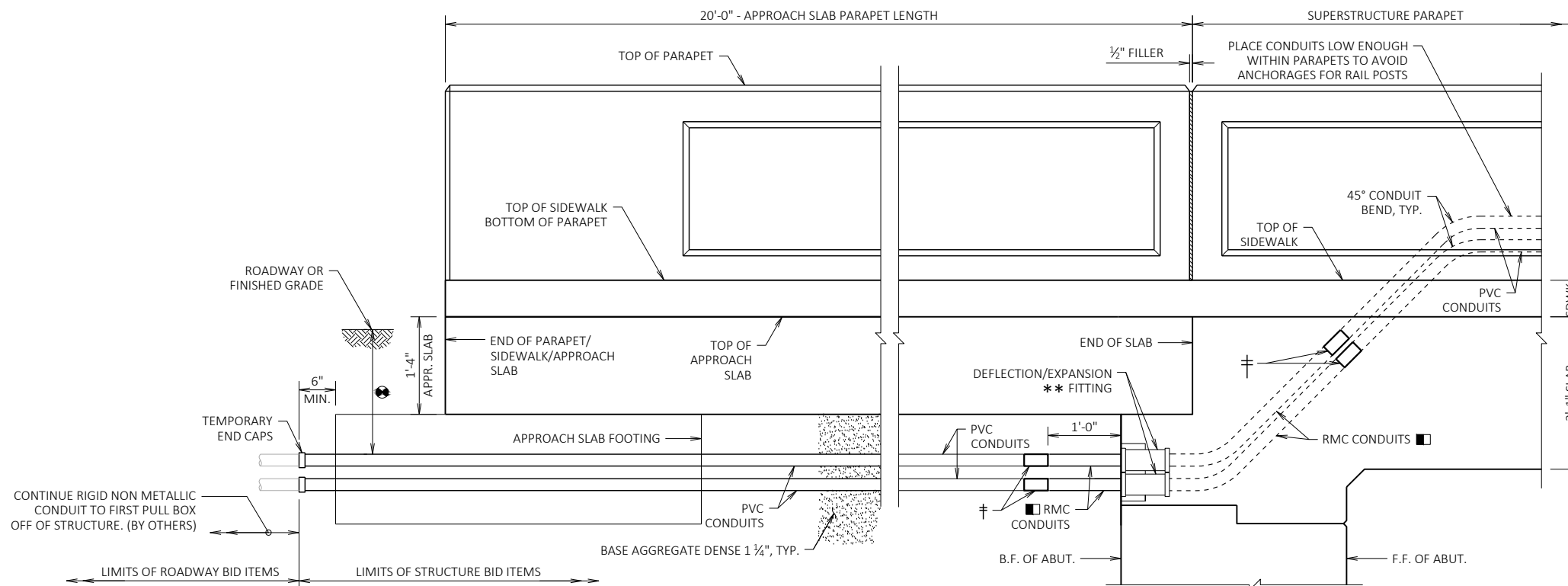
(RAILING NOT SHOWN FOR CLARITY)
(DETAILS AT WING 1 SHOWN, OTHERS SIMILAR)

NOTES

- CONDUIT SHALL BE EMBEDDED 2" CLEAR.
- USE 2" DIA. RIGID NONMETALLIC CONDUIT (PVC) UNLESS NOTED OTHERWISE.
- CONDUIT FITTINGS, CONDUIT BENDS, AND ADAPTER FITTINGS INCIDENTAL TO CONDUIT WORK.
- CONDUIT BENDS SHALL CONFORM TO THE NATIONAL ELECTRIC CODE.
- 2'-0" MIN. CONDUIT COVER UNDER ROADWAYS, 1'-6" OTHERWISE. CONDUIT COVER SHOULD NOT EXCEED 3'-0"
- PROVIDE JUNCTION BOXES FROM THE APPROVED PRODUCTS LIST.
- LOCATE CONDUIT IN PARAPET TO AVOID RAIL ANCHORAGES.
- AN ADDITIONAL CONDUIT IS PROVIDED AT EACH PARAPET FOR FUTURE USE.

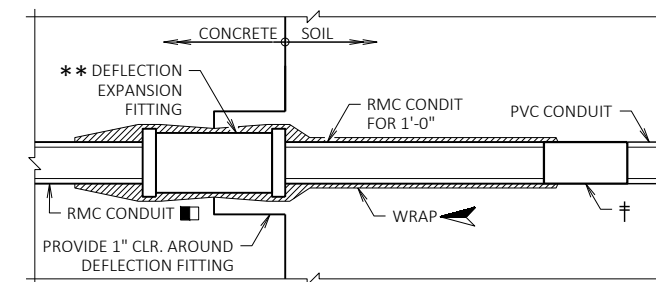
LEGEND

- USE 2" DIA. RIGID METALLIC (RMC) CONDUIT AT FITTINGS. PROVIDE RMC FOR 3'-0" MIN. ON EACH SIDE OF JOINT OPENINGS UNLESS NOTED OTHERWISE.
- † NONMETALLIC CONDUIT TO METALLIC CONDUIT ADAPTER FITTING (UL OR NRTL LISTED FOR ELECTRICAL USE SHALL BE USED)
- ☞ SPONGE RUBBER WRAP TO BE AASHTO M153, TYPE 1 OR EQUIVALENT - 3/4" MINIMUM THICKNESS. PROVIDE WRAP FOR THE ENTIRE LENGTH OF THE FITTING OR AS SHOWN. SPONGE RUBBER WRAP INCIDENTAL TO "CONDUIT RIGID METALLIC 2-INCH".
- ** DEFLECTION/EXPANSION FITTING REQUIREMENTS (IF USED):
UP TO 3/4" CONDUIT CONTRACTION OR EXPANSION AND UP TO 30 DEGREES OF ANGULAR MISALIGNMENT IN ANY DIRECTION WITH BONDING JUMPER
- ▲ PROVIDE PVC CONDUIT SLEEVES, OR APPROVED ALTERNATE METHOD BY ENGINEER, AT LOCATIONS WHERE CONDUIT RUNS THROUGH EAST APPROACH SLAB FOOTING. COST OF SLEEVE IS INCIDENTAL TO "CONCRETE MASONRY" BID ITEM.



OUTSIDE ELEVATION OF PARAPET AT APPROACH SLAB SHOWING CONDUIT SYSTEM

WING 1 CORNER SHOWN, OTHERS SIMILAR
(RAILING AND WING NOT SHOWN FOR CLARITY)



DEFLECTION/EXPANSION FITTING

THIS DETAIL ACCOMMODATES A MAXIMUM OF 3/4" TOTAL MOVEMENT AND UP TO 30 DEGREES OF ANGULAR MISALIGNMENT IN ANY DIRECTION.
BOND JUMPER NOT SHOWN FOR CLARITY
(CONCRETE TO SOIL FITTING)

NO.	DATE	REVISION	BY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

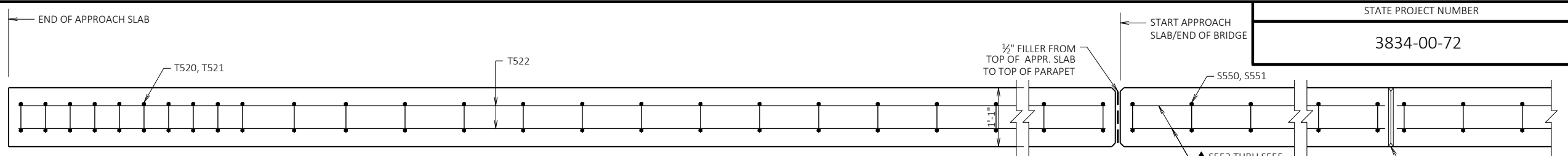
STRUCTURE B-51-154

DRAWN BY PKF PLANS CK'D. BH

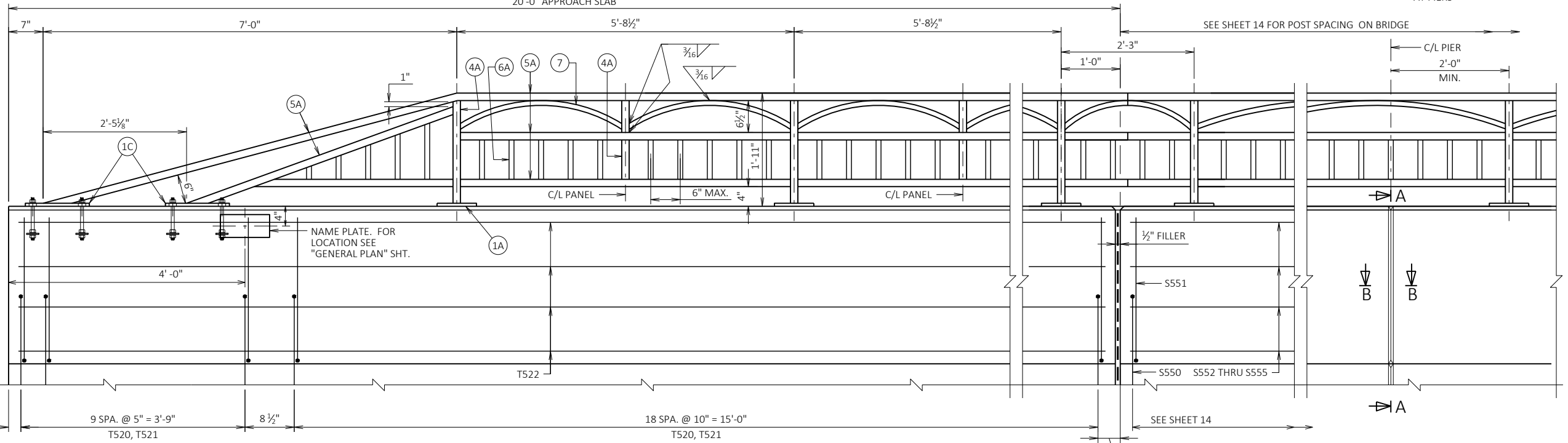


CONDUIT DETAILS

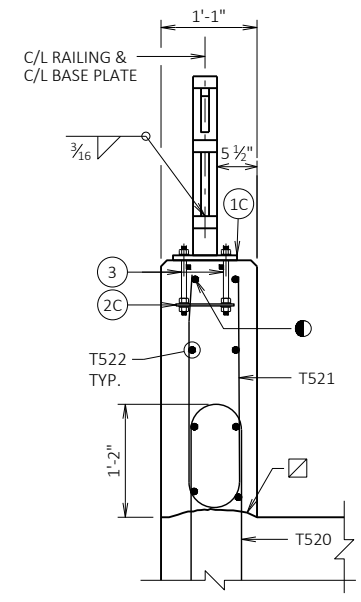
SHEET 21 OF 28



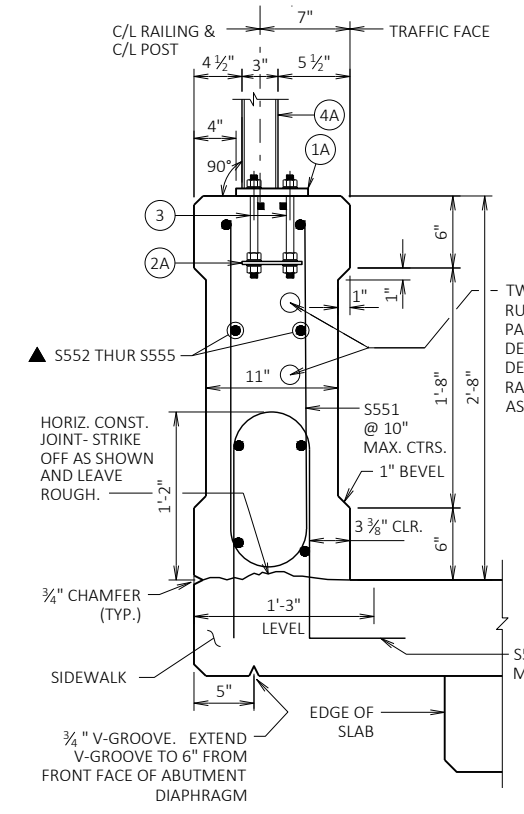
PLAN



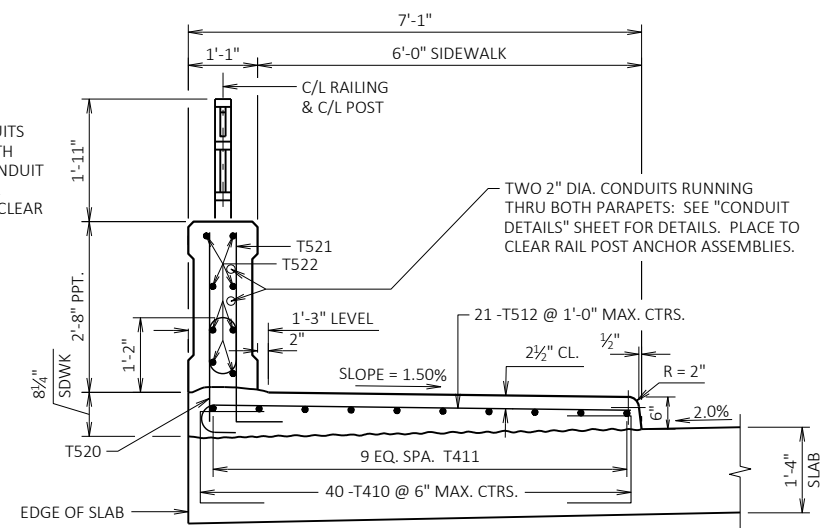
ELEVATION OF PARAPET



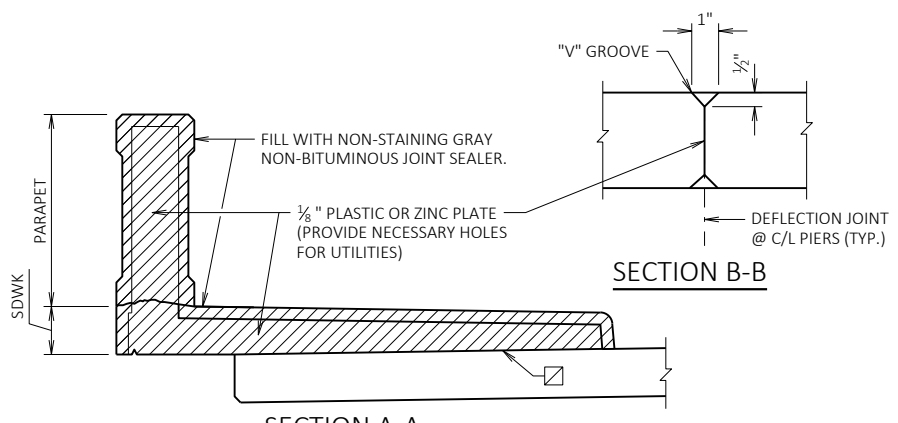
END VIEW



SECTION THRU PARAPET ON BRIDGE



SECTION AT APPROACH SLAB



SECTION A-A

SECTION B-B

(SHOWING DEFLECTION JOINT IN PARAPET AND SIDEWALK.)

WHEN PARAPETS ARE POURED CONTINUOUSLY FROM END TO END, THEY SHALL BE SEPARATED AT THE DEFLECTION JOINTS BY A PIECE OF 3/8\"/>

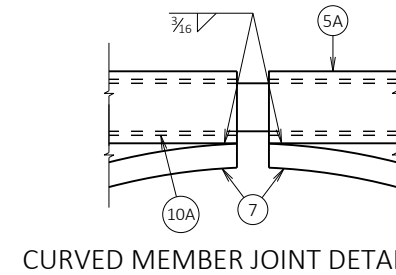
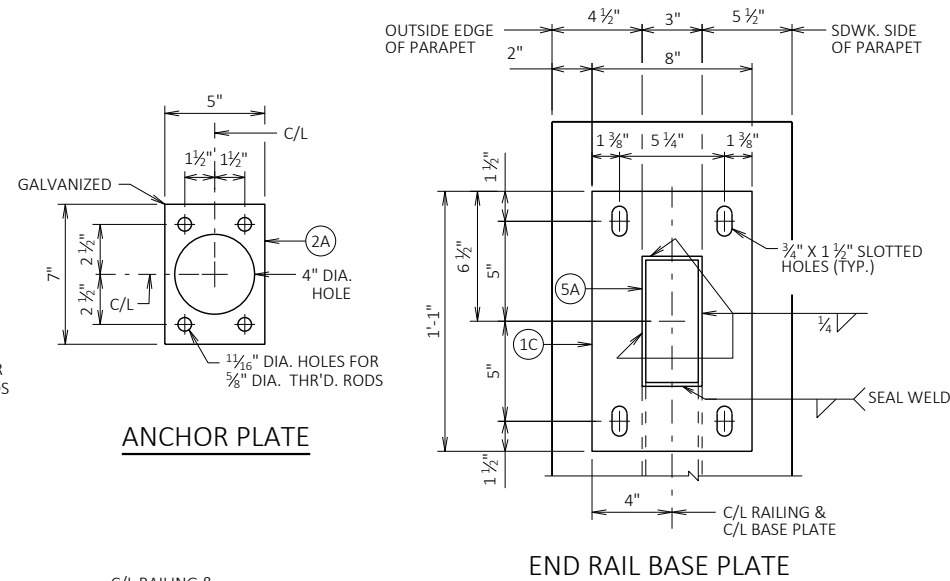
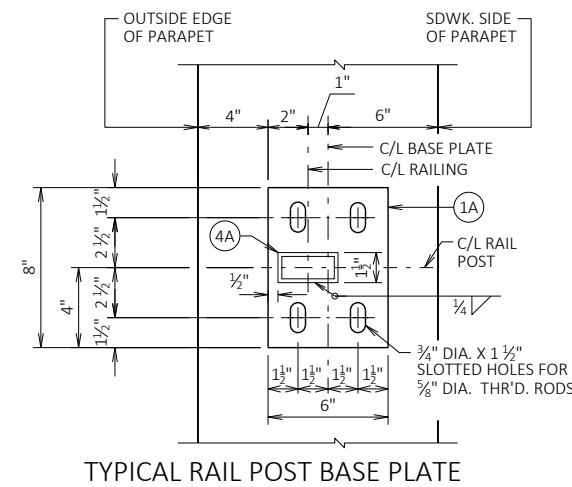
NOTES

- REINFORCEMENT FOR THE PARAPET ON THE BRIDGE IS INCLUDED IN THE BAR TABLE ON SHEET 20.
- REINFORCEMENT FOR THE PARAPET ON THE APPROACH SLAB IS INCLUDED IN THE BAR TABLE ON SHEET 26.
- ADJUST LOCATIONS OF BARS TO ALLOW PLACEMENT OF ANCHOR ASSEMBLY FOR RAILING.

LEGEND

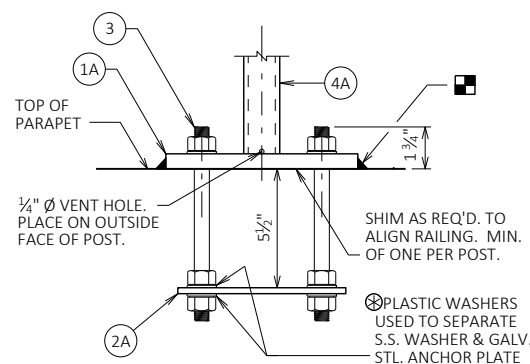
- ▲ OPTIONAL CONSTRUCTION JOINTS IN THE PARAPETS MAY BE USED. RUN BAR REINFORCEMENT THRU THE JOINT. LAP LONGITUDINAL BARS A MINIMUM OF 1'-5\"/>
- ☐ HORIZ. CONSTRUCTION JOINT - STRIKE OFF AS SHOWN AND LEAVE ROUGH.
- * ± 0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- WHEN ADHESIVE ANCHORS ARE USED, FIELD BEND AND/OR DISPLACE TO AVOID HITTING LONGITUDINAL BAR WHEN DRILLING FOR ADHESIVE ANCHORS.

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-51-154			
DRAWN BY PKF		PLANS CK'D. BH	
PARAPET & RAILING - 1			SHEET 22 OF 28

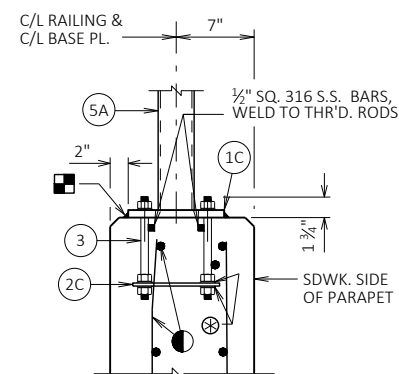


LEGEND

- 1A PLATE 5/8" X 6" X 8" WITH 3/4" X 1 1/2" SLOTTED HOLES.
- 1C PLATE 5/8" X 8" X 1'-1" WITH 3/4" X 1 1/2" SLOTTED HOLES.
- 2A 1/4" X 5" X 7" ANCHOR PLATE WITH 1 1/2" DIA. HOLES FOR THR'D. RODS NO. 3.
- 2C 1/4" X 2 1/2" X 7 3/4" ANCHOR PLATE WITH 1 1/2" DIA. HOLES FOR THR'D. RODS NO. 3.
- 3 5/8" DIA. X 9" LONG, TYPE 316 STAINLESS STEEL THREADED RODS (MIN. TENSILE STRENGTH = 70 KSI) WITH NUT AND WASHERS OF SAME ALLOY GROUP. ALTERNATIVE ANCHORAGE: CONCRETE ADHESIVE ANCHORS 3/8"-INCH. EMBED 7" IN CONCRETE FOR RAIL POSTS. EMBED 5" IN CONCRETE FOR END RAILS. ADHESIVE ANCHORS SHALL CONFORM TO SECTION 502.2.12 AND 502.3.14 OF THE STANDARD SPECIFICATIONS.
- 4A STRUCTURAL TUBING 3" X 1 1/2" X 3/16". PLACE VERTICAL. WELD TO NO. 1 & 5.
- 5A STRUCTURAL TUBING 3" X 1 1/2" X 3/16" RAILS. WELD TO NO. 1 & NO. 4. INSIDE OF TUBE TO BE PAINTED AT ALL FIELD ERECTION & EXPANSION JOINTS.
- 6A BAR 1" X 1" PICKETS. WELD TO NO. 5. PLACE VERTICAL. SPACE AT 6" MAX. C/L TO C/L.
- 7 BAR 1" X 1". BEND TO REQUIRED RADIUS. WELD TO NO. 4 & 5.
- 9A RECTANGULAR SLEEVE FABRICATED FROM 3/16" PLATES. PROVIDE "SLIDING FIT".
- 10A RECTANGULAR SLEEVE FABRICATED FROM 3/16" PLATES. (1'-4" @ FIELD ERECTION JTS.)

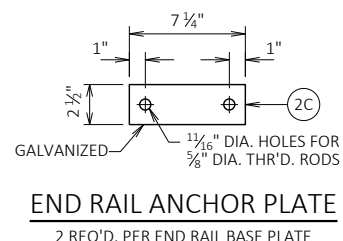


NOTE: ANCHOR PLATE NOT REQUIRED WHEN ADHESIVE ANCHORS ARE USED.

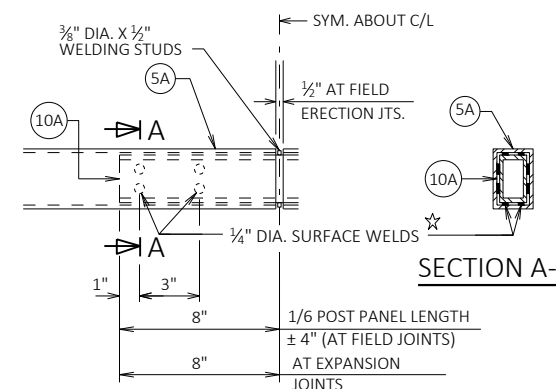
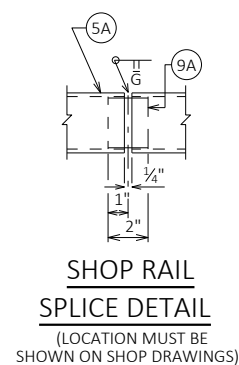
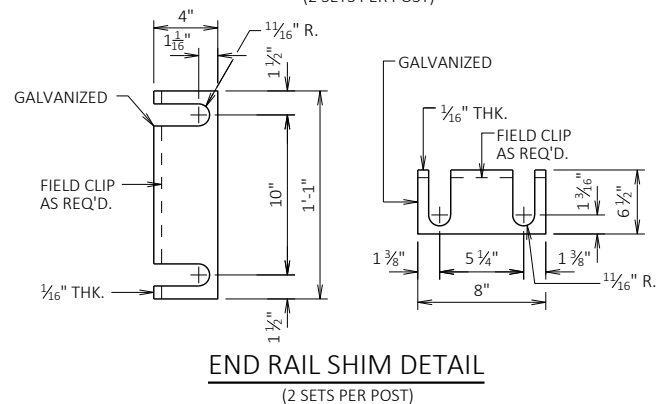
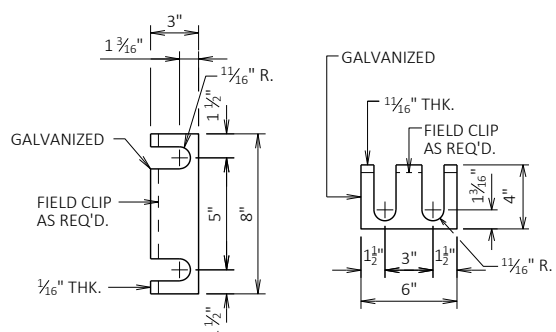


NOTE: ANCHOR PLATES NOT REQ'D. WHEN ADHESIVE ANCHORS ARE USED.

WHEN ADHESIVE ANCHORS ARE USED, FIELD BEND AND/OR DISPLACE TO AVOID HITTING LONGITUDINAL BAR WHEN DRILLING FOR ADHESIVE ANCHORS.



2 REQ'D. PER END RAIL BASE PLATE

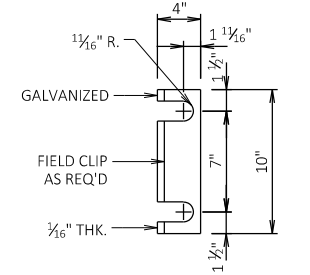
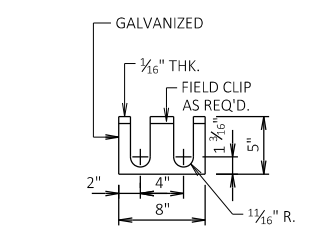
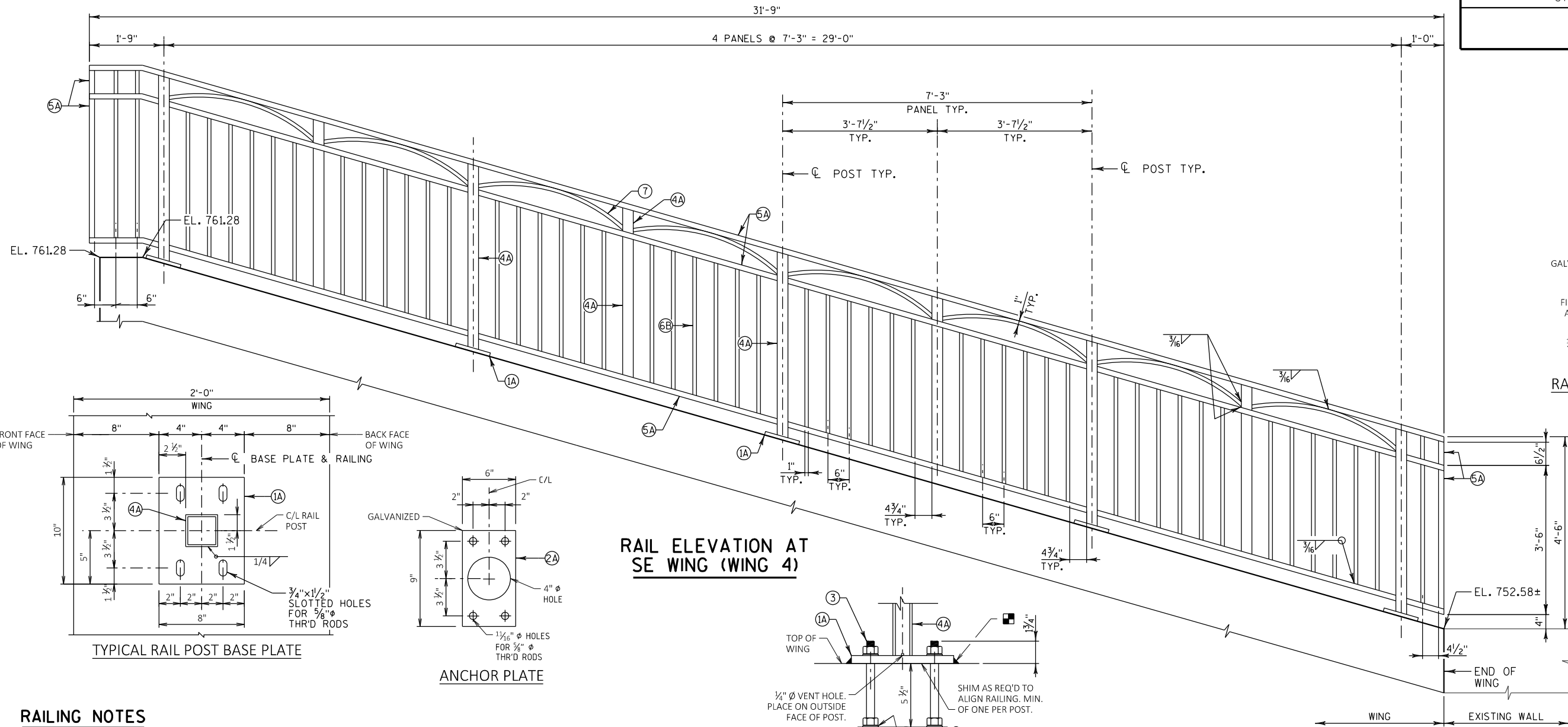


MIN. 5/8" FLAT SURFACE DIA. PUNCHINGS OR STUDS MAY BE USED AS AN ALTERNATE.

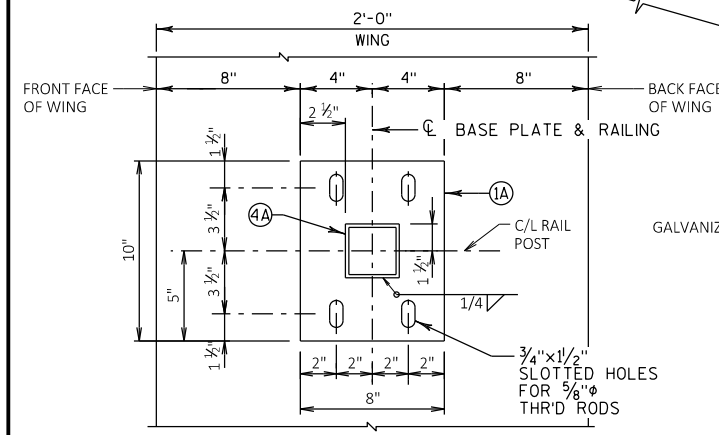
RAILING NOTES

- BID ITEM SHALL BE "RAILING STEEL TYPE C3", WHICH SHALL INCLUDE ALL STEEL ITEMS SHOWN.
- POST BASE PLATES SHALL BE FLAT WITH ALL SURFACES SMOOTH AND FREE FROM WARP AND ALL EDGES SMOOTH, STRAIGHT AND VERTICAL. ALL PLATE CUTS SHALL BE MACHINE OR MACHINE FLAME CUTS.
- ALL PLATES, BARS, AND RECTANGULAR SLEEVES SHALL CONFORM TO ASTM A709 GRADE 36. ALL STRUCTURAL TUBING SHALL CONFORM TO ASTM A500 GRADE B.
- ANCHORAGES SHALL BE ACCURATELY PLACED TO PROVIDE CORRECT ALIGNMENT OF RAILING. SET NORMAL TO GRADE.
- CUT BOTTOM OF POST TO MAKE POST VERTICAL IN BOTH TRANSVERSE AND LONGITUDINAL DIRECTION.
- STEEL SHIMS SHALL BE PROVIDED & USED UNDER BASE PLATES WHERE REQUIRED FOR ALIGNMENT, AND SHALL BE GALVANIZED.
- CAULK AROUND PERIMETER OF BASE PLATES, NO. 1, AND FILL BOLT SLOT OPENINGS IN SHIMS AND BASE PLATES WITH NON-STAINING GRAY NON-BITUMINOUS JOINT SEALER.
- ALL JOINTS AND RECESSES IN CONCRETE PARAPET ARE TO BE VERTICAL.
- ALL MATERIAL (EXCEPT NO. 3) SHALL BE GALVANIZED AFTER FABRICATION. PRIOR TO GALVANIZING, THE STEEL RAILING SHALL BE GIVEN A NO. 6 BLAST CLEANING PER SSPC SPECIFICATIONS. PAINT OVER GALVANIZING WITH AN APPROVED TIE COAT AND TOP COAT AS SPECIFIED IN THE CONTRACT DOCUMENTS. THE RAILING SHALL BE PAINTED AMS STD. COLOR NO. 27038, BLACK.
- VENT HOLES SHALL BE DRILLED IN POST AND RAIL MEMBERS AS REQUIRED TO FACILITATE GALVANIZING AND DRAINAGE.
- RAILING SHALL BE FABRICATED IN LENGTHS THAT INCLUDE 3 OR 4 POSTS.
- TOUCH-UP PAINTING TO BE DONE AT COMPLETION OF STEEL RAILING INSTALLATION TO THE SATISFACTION OF THE ENGINEER AT NO EXTRA COST.

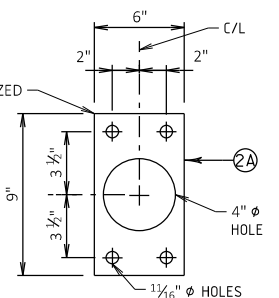
NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-51-154			
DRAWN BY		PKE	PLANS CK'D. BH
PARAPET & RAILING - 2			SHEET 23 OF 28



RAIL POST SHIM DETAIL
(2 SETS PER POST)

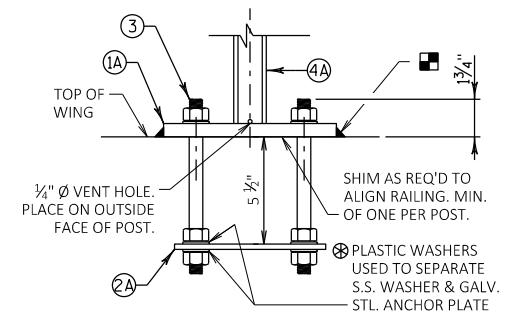


TYPICAL RAIL POST BASE PLATE



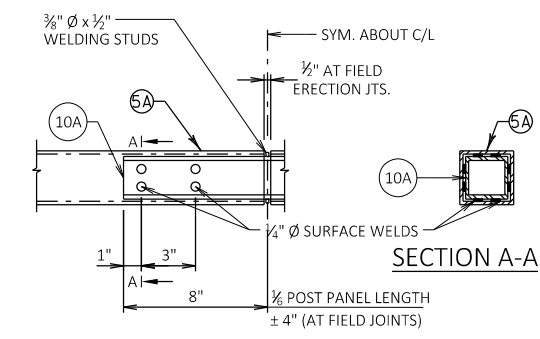
ANCHOR PLATE

RAIL ELEVATION AT SE WING (WING 4)



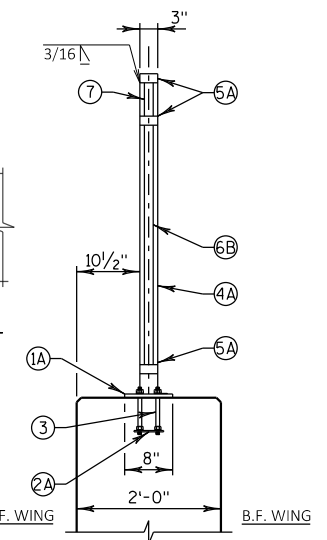
ANCHORAGE FOR RAIL POSTS

NOTE: ANCHOR PLATE NOT REQUIRED WHEN ADHESIVE ANCHORS ARE USED.



FIELD ERECTION JOINT DETAIL

*MIN. 5/8" FLAT SURFACE DIA. PUNCHINGS OR STUDS MAY BE USED AS AN ALTERNATE.



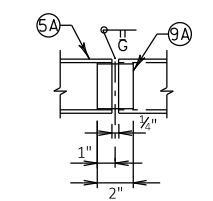
SECTION THRU WING
(ADJUST REINFORCEMENT AT ANCHORAGES)

RAILING NOTES

- BID ITEM SHALL BE "RAILING STEEL PEDESTRIAN TYPE C3", WHICH SHALL INCLUDE ALL STEEL ITEMS SHOWN.
- POST BASE PLATES SHALL BE FLAT WITH ALL SURFACES SMOOTH AND FREE FROM WARP AND ALL EDGES SMOOTH, STRAIGHT AND VERTICAL. ALL PLATE CUTS SHALL BE MACHINE OR MACHINE FLAME CUTS.
- ALL PLATES, BARS, AND RECTANGULAR SLEEVES SHALL CONFORM TO ASTM A709 GRADE 36. ALL STRUCTURAL TUBING SHALL CONFORM TO ASTM A500 GRADE B.
- ANCHORAGES SHALL BE ACCURATELY PLACED TO PROVIDE CORRECT ALIGNMENT OF RAILING.
- CUT BOTTOM OF POST TO MAKE POST VERTICAL IN BOTH TRANSVERSE AND LONGITUDINAL DIRECTION.
- STEEL SHIMS SHALL BE PROVIDED & USED UNDER BASE PLATES WHERE REQUIRED FOR ALIGNMENT, AND SHALL BE GALVANIZED.
- CAULK AROUND PERIMETER OF BASE PLATES, NO. 1, AND FILL BOLT SLOT OPENINGS IN SHIMS AND BASE PLATES WITH NON-STAINING GRAY NON-BITUMINOUS JOINT SEALER.
- ALL MATERIAL (EXCEPT NO. 3) SHALL BE GALVANIZED AFTER FABRICATION. PRIOR TO GALVANIZING, THE STEEL RAILING SHALL BE GIVEN A NO. 6 BLAST CLEANING PER SSPC SPECIFICATIONS. PAINT OVER GALVANIZING WITH AN APPROVED TIE COAT AND TOP COAT AS SPECIFIED IN THE CONTRACT DOCUMENTS. THE RAILING SHALL BE PAINTED AMS STD. COLOR NO. 27038, BLACK.
- VENT HOLES SHALL BE DRILLED IN POST AND RAIL MEMBERS AS REQUIRED TO FACILITATE GALVANIZING AND DRAINAGE.
- RAILING SHALL BE FABRICATED IN LENGTHS THAT INCLUDE 3 OR 4 POSTS.
- TOUCH-UP PAINTING TO BE DONE AT COMPLETION OF STEEL RAILING INSTALLATION TO THE SATISFACTION OF THE ENGINEER AT NO EXTRA COST.

LEGEND

- 1A) PLATE 1" X 8" X 10" WITH 3/4" X 1/2" SLOTTED HOLES.
- 2A) 1/4" X 6" X 9" ANCHOR PLATE WITH 1/16" DIA. HOLES FOR THRD. RODS NO. 3.
- 3) 3/8" DIA. X 9" LONG, TYPE 316 STAINLESS STEEL THREADED RODS (MIN. TENSILE STRENGTH = 70 KSI) WITH NUT AND WASHERS OF SAME ALLOY GROUP. ALTERNATIVE ANCHORAGE: CONCRETE ADHESIVE ANCHORS 5/8"-INCH. EMBED 7" IN CONCRETE FOR RAIL POSTS. EMBED 5" IN CONCRETE FOR END RAILS. ADHESIVE ANCHORS SHALL CONFORM TO SECTIONS 502.2.12 AND 502.3.14 OF THE STANDARD SPECIFICATIONS.
- 4A) STRUCTURAL TUBING 3" X 3" X 3/16". PLACE VERTICAL. WELD TO NO. 1 & NO. 5.
- 5A) STRUCTURAL TUBING 3" X 1 1/2" X 3/16" RAILS, WELD TO NO. 1 & NO. 4, INSIDE OF TUBE TO BE PAINTED AT ALL FIELD ERECTION & EXPANSION JOINTS.
- 6B) BAR 1" X 1/2" PICKETS, WELD TO NO. 5 (SPACE AT 6" MAX. C/L TO C/L SPACING). PLACE VERTICAL.
- 7) BAR 1" X 1/2". BEND TO REQUIRED RADIUS. WELD TO NO. 4 & NO. 5.
- 9A) RECTANGULAR SLEEVE FABRICATED FROM 3/16" PLATES. PROVIDE "SLIDING FIT".
- 10A) RECTANGULAR SLEEVE FABRICATED FROM 3/16" PLATES. (1'-4" @ FIELD ERECTION JTS.)



SHOP RAIL SPLICE DETAIL

(LOCATION MUST BE SHOWN ON SHOP DRAWINGS)

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-51-154			
DRAWN BY TKB		PLANS CK'D. BH	
SE WING RAILING			SHEET 24 OF 28

BILL OF BARS - STRUCTURAL APPROACH SLABS

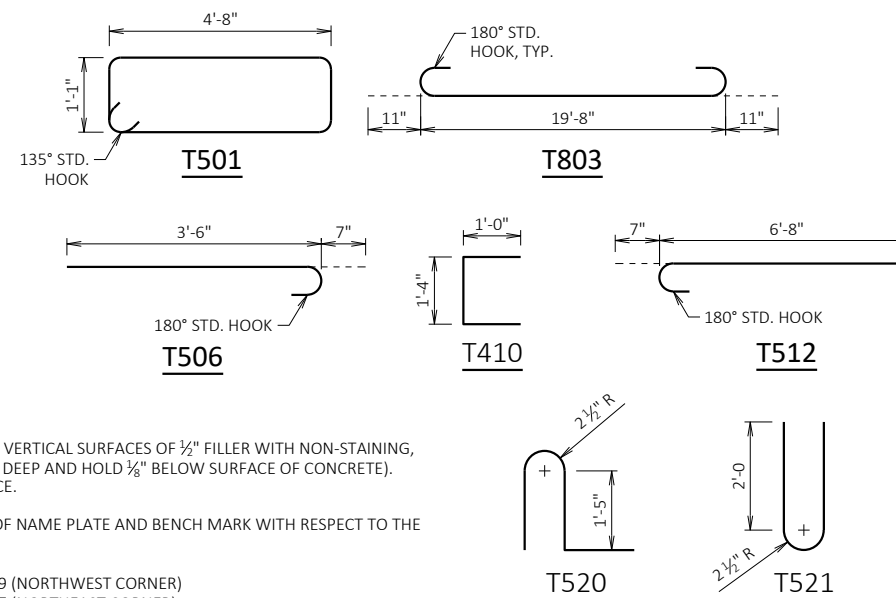
BAR MARK	# REQ'D WEST	# REQ'D EAST	LENGTH	BENT	BAR SERIES	LOCATION
STAINLESS STEEL BARS						TOTAL WEIGHT = 450 LBS
SS601	49	49	3'-0"			STRUCTURE TO APPR. SLAB LONGIT.

BAR MARK	# REQ'D WEST	# REQ'D EAST	LENGTH	BENT	BAR SERIES	LOCATION
COATED BARS						TOTAL WEIGHT = 22,950 LBS
T501	49	49	12'-2"	X		SLAB - FTG., STIRRUP LONGIT.
T802	12	12	47'-10"			SLAB - FTG. TRANS.
T803	77	77	21'-6"	X		SLAB - BTM. LONGIT.
T504	49	49	19'-8"			SLAB - TOP LONGIT.
T505	42	42	47'-10"			SLAB - TOP & BTM. TRANS.
T506	40	40	4'-1"	X		SLAB - TOP TRANS.
T410	80	80	3'-2"	X		SIDEWALK VERT.
T411	20	20	19'-7"			SIDEWALK LONG.
T512	42	42	7'-3"	X		SIDEWALK TRANS.
T520	58	58	4'-4"	X		PARAPETS VERT.
T521	58	58	4'-9"	X		PARAPETS VERT.
T522	16	16	19'-7"			PARAPETS LONG.

THE FIRST DIGIT OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE.

DIMENSIONS IN BENDING DETAILS ARE OUT-TO-OUT OF BAR

BOTH APPROACH SLABS ARE INCLUDED IN THIS BILL OF BARS



LEGEND

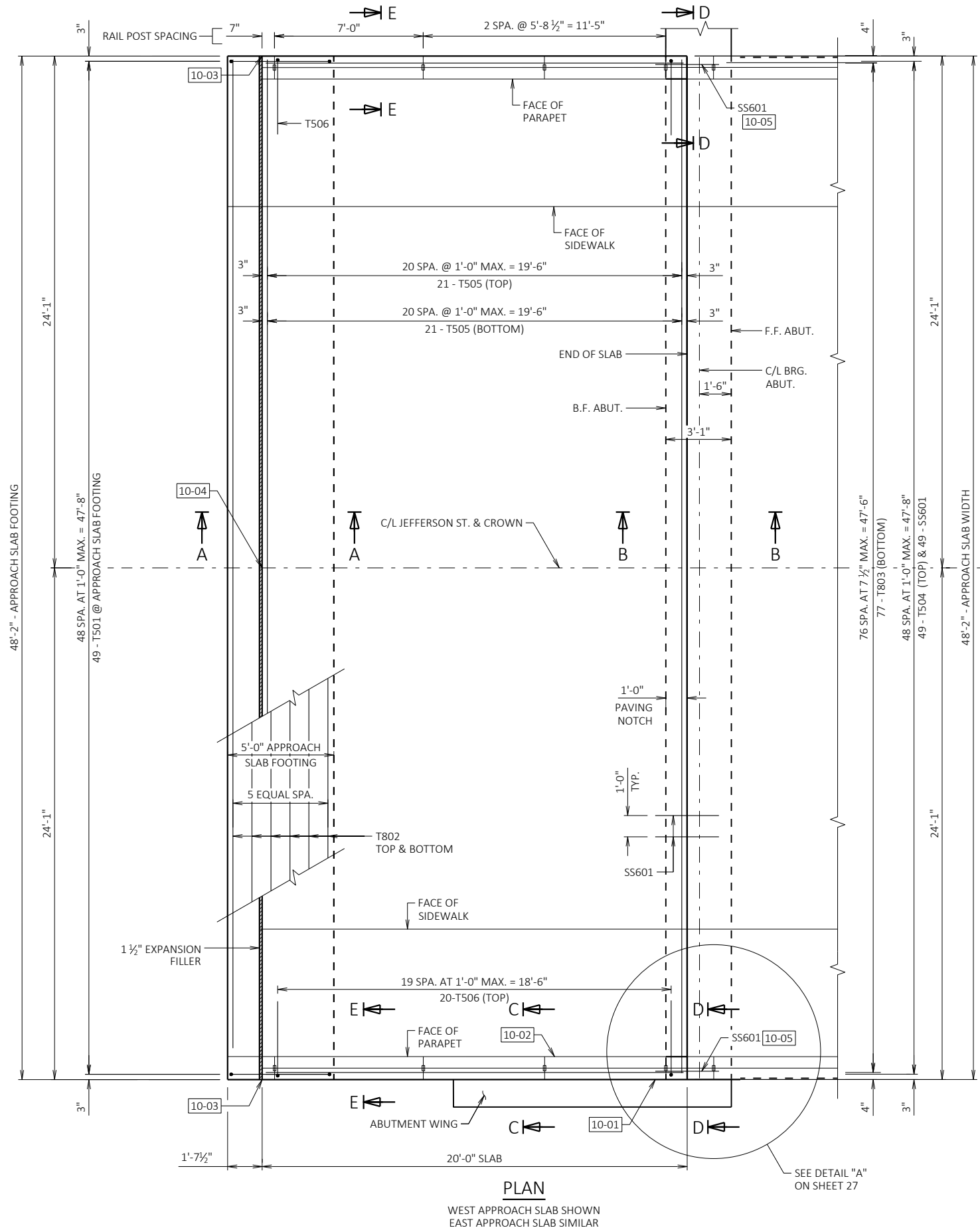
- 10-01 SEAL ALL EXPOSED HORIZONTAL AND VERTICAL SURFACES OF 1/2" FILLER WITH NON-STAINING, NON-BITUMINOUS JOINT SEALER, (1" DEEP AND HOLD 1/2" BELOW SURFACE OF CONCRETE). COLOR TO MATCH ADJACENT SURFACE.
- 10-02 SEE PARAPET SHEET FOR LOCATION OF NAME PLATE AND BENCH MARK WITH RESPECT TO THE END OF PARAPET.
- 10-03 STA. 11+93.42, 24.08' LEFT, EL. 760.59 (NORTHWEST CORNER)
STA. 13+96.58, 24.08' LEFT, EL. 760.87 (NORTHEAST CORNER)
STA. 11+93.42, 24.08' RIGHT, EL. 760.59 (SOUTHWEST CORNER)
STA. 13+96.58, 24.08' RIGHT, EL. 760.87 (SOUTHEAST CORNER)
- 10-04 STA. 11+93.42, EL. 761.07 (WEST END)
STA. 13+96.58, EL. 761.35 (EAST END)
- 10-05 INCLUDED IN ITEM "BAR STEEL REINFORCEMENT HS STAINLESS STRUCTURES".

DESIGN DATA

CONCRETE STRENGTH, f'_c _____ 4,000 P.S.I.
 BAR STEEL REINFORCEMENT, GRADE 60, f_y _____ 60,000 P.S.I.
 ALLOWABLE SOIL BEARING PRESSURE _____ 2,000 P.S.F.

NOTE:

SEE SHEET 27 FOR SECTIONS A-A THRU E-E.



PLAN

WEST APPROACH SLAB SHOWN
 EAST APPROACH SLAB SIMILAR

NO.	DATE	REVISION	BY

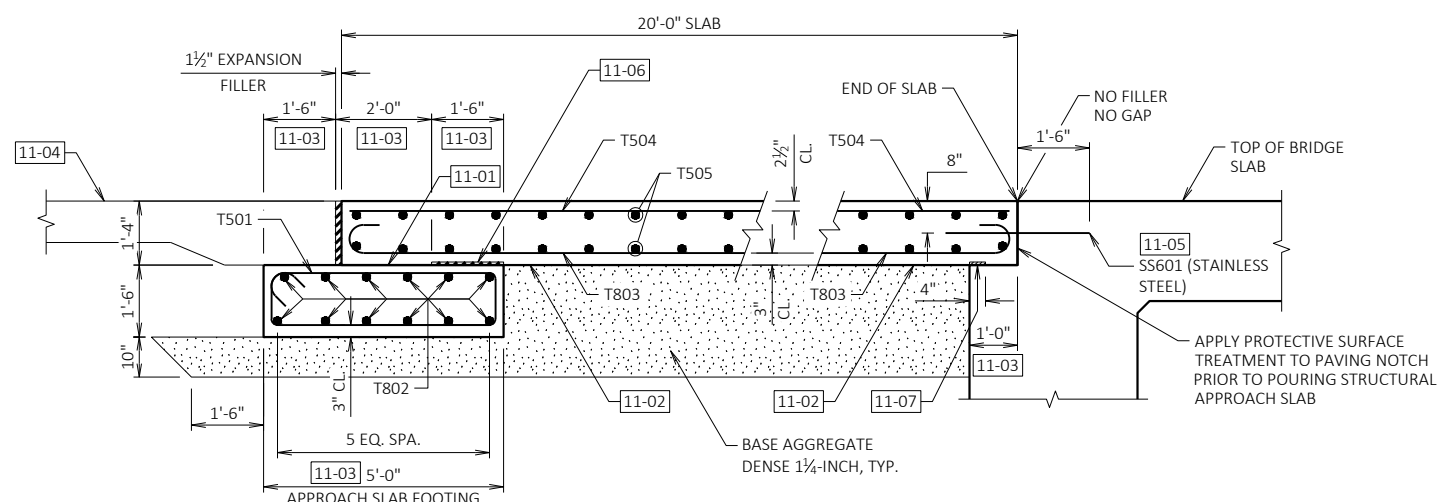
STATE OF WISCONSIN
 DEPARTMENT OF TRANSPORTATION

STRUCTURE B-51-154

DRAWN BY PKF PLANS CK'D. BH

STRUCTURAL APPROACH SLAB

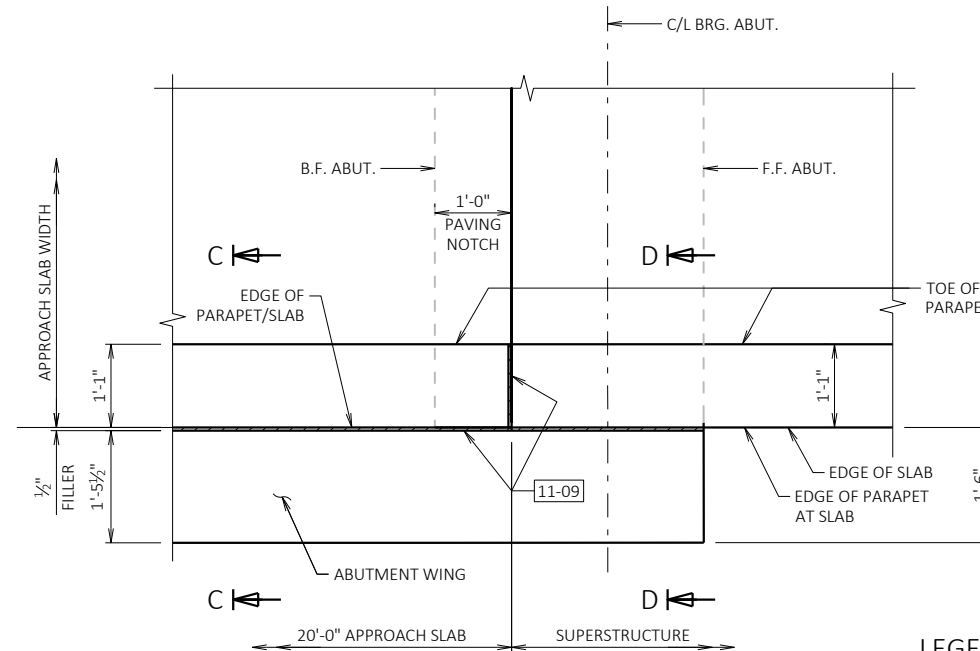
SHEET 26 OF 28



SECTION A-A

SECTION B-B

SECTION THRU APPROACH SLAB



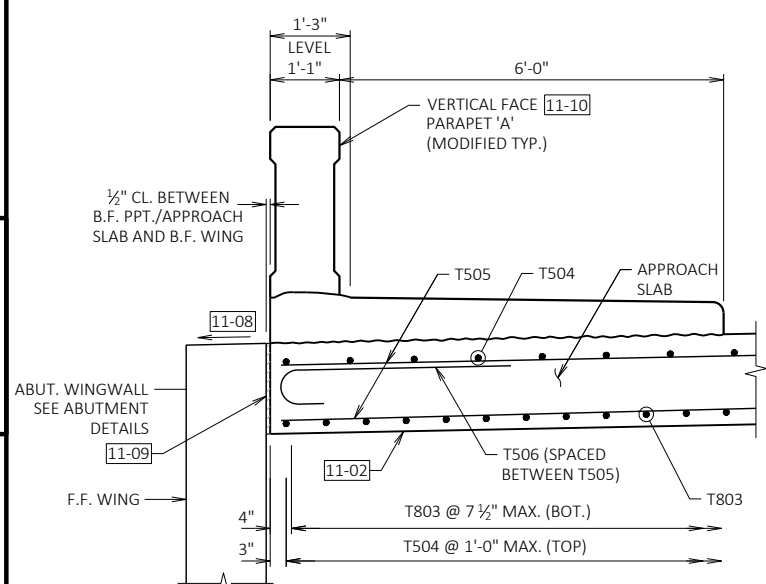
DETAIL "A"

LEGEND

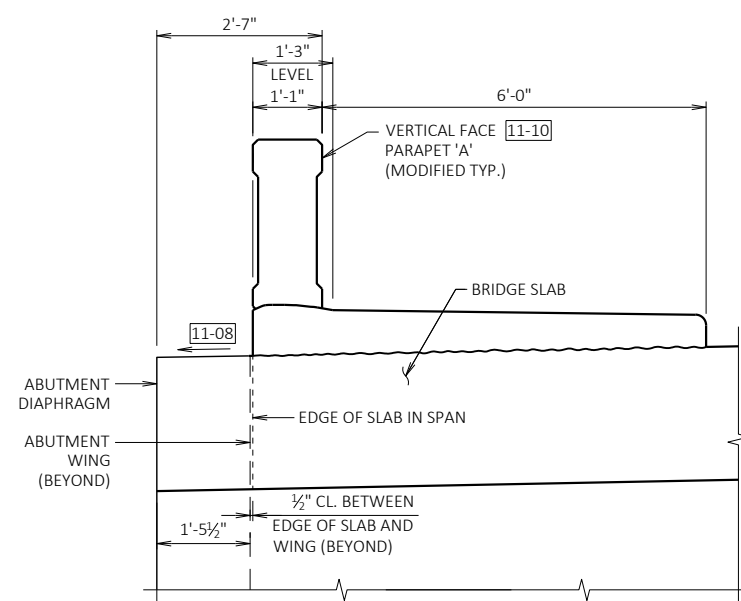
- 11-01 STEEL TROWEL TOP SURFACE OF FOOTING AND PLACE MULTIPLE LAYERS (0.03" MIN. TOTAL THICK) OF POLYETHYLENE SHEETS OVER THE ENTIRE TOP OF FOOTING.
- 11-02 PLACE MULTIPLE LAYERS (0.03" MIN. TOTAL THICK) OF POLYETHYLENE SHEETS OVER THE ENTIRE TOP OF SUBGRADE BENEATH SLAB.
- 11-03 MEASURED NORMAL TO ABUTMENT.
- 11-04 ROADWAY APPROACH PAVEMENT.
- 11-05 THE BID ITEM FOR SS601 SHALL BE "BAR STEEL REINFORCEMENT HS STAINLESS STRUCTURES".
- 11-06 3/4" PREFORMED JOINT FILLER ACCORDING TO STANDARD SPEC. 502.2.7 (1'-6" WIDE x 48'-2" LONG).
- 11-07 3/4" PREFORMED JOINT FILLER ACCORDING TO STANDARD SPEC. 502.2.7 (4" WIDE x 48'-2" LONG).
- 11-08 SLOPE TO DRAIN.
- 11-09 SEAL ALL EXPOSED HORIZONTAL AND VERTICAL SURFACES OF 1/2" FILLER WITH NON-STAINING, NON-BITUMINOUS JOINT SEALER (1" DEEP AND HOLD 3/8" BELOW SURFACE OF CONCRETE). COLOR TO MATCH ADJACENT SURFACE.
- 11-10 SEE SHEET 22 FOR REINFORCEMENT, LOCATION OF NAME PLATE AND BENCH MARK WITH RESPECT TO THE END OF PARAPET, ETC.
- 11-11 TRANSITION FILL FROM TOP OF SIDEWALK TO TOP OF WING AS DIRECTED BY ENGINEER.

NOTE:

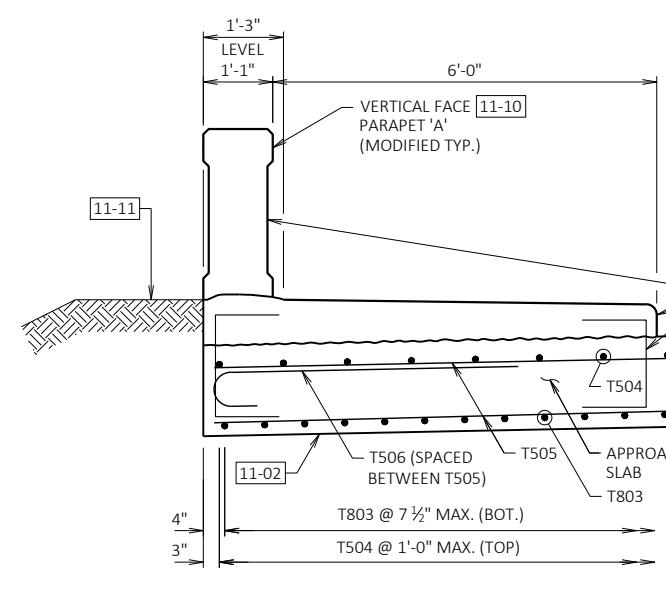
SEE SHEET 26 FOR LOCATIONS OF SECTIONS & DETAILS.



SECTION C-C



SECTION D-D



SECTION E-E

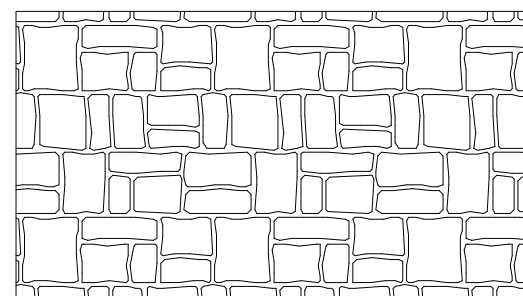
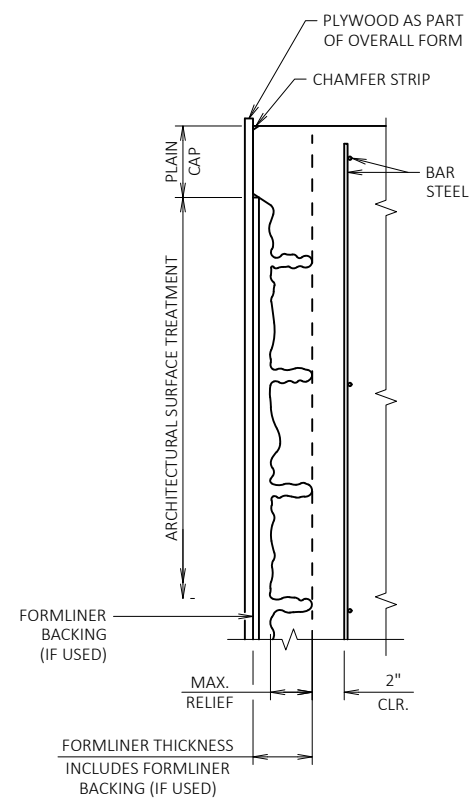
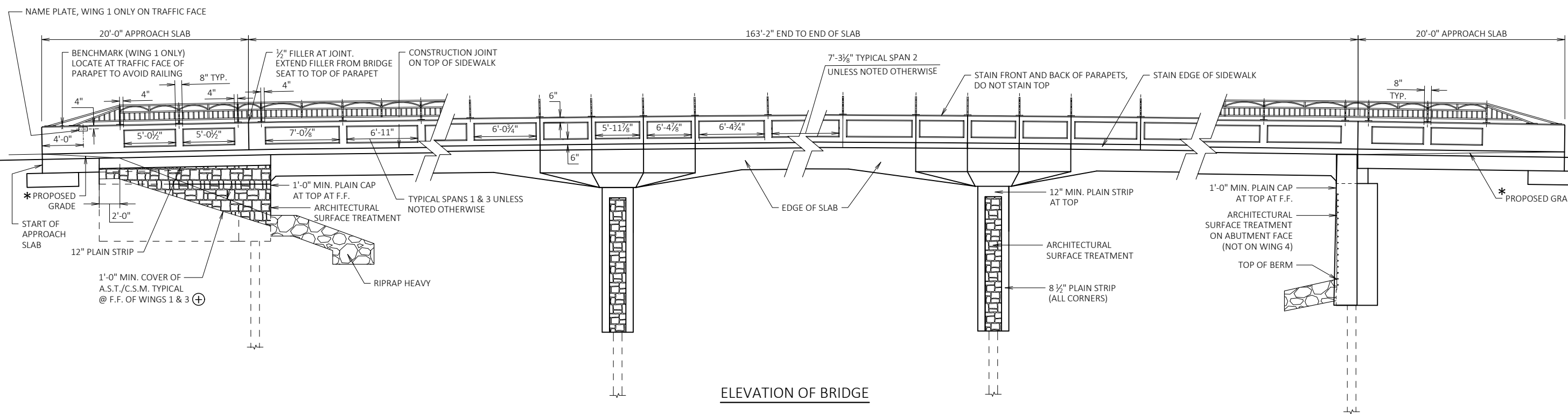
SEE SHEET 22 FOR SIDEWALK & PARAPET REINFORCEMENT

8

8

NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-51-154			
DRAWN BY PKF		PLANS CK'D. BH	
STRUCTURAL APPROACH SLAB DETAILS			SHEET 27 OF 28





FORMLINER NOTES

FORMLINER PATTERN SHALL MATCH STRUCTURE B-51-46, STATE STREET OVER THE FOX RIVER IN THE CITY OF BURLINGTON. FORMLINER PATTERN SHOWN IS GENERIC AND MAY NOT REPRESENT THE ACTUAL PATTERN TO BE USED.

FORMLINER COURSING ON ABUTMENTS, WINGS, & PIERS SHALL BE LEVEL.

THE FORMLINER COURSING ON THE WINGS SHALL BE VERTICALLY ALIGNED WITH THE FORMLINER COURSING ON THE FRONT OF THE ABUTMENT.

THE FORMLINER PATTERN SHALL BE CONTINUOUS ACROSS CONSTRUCTION JOINTS.

WRAPAROUND/MATCH FORMLINER PATTERN AT CORNERS.

ADDITIONAL FORMLINER DETAILS ARE SHOWN ON THE ABUTMENT AND PIER SHEETS.

NOTES

STAIN COLORS SHALL MATCH STRUCTURE B-51-46, STATE STREET OVER THE FOX RIVER.

CONCRETE STAINING LIMITS: PARAPETS, EDGE OF SIDEWALKS, AND PLAIN STRIPS AT ABUTMENTS AND PIERS.

CONCRETE STAINING MULTI-COLOR LIMITS: ABUTMENTS AND PIERS, EQUIVALENT TO ARCHITECTURAL SURFACE TREATMENT LIMITS.

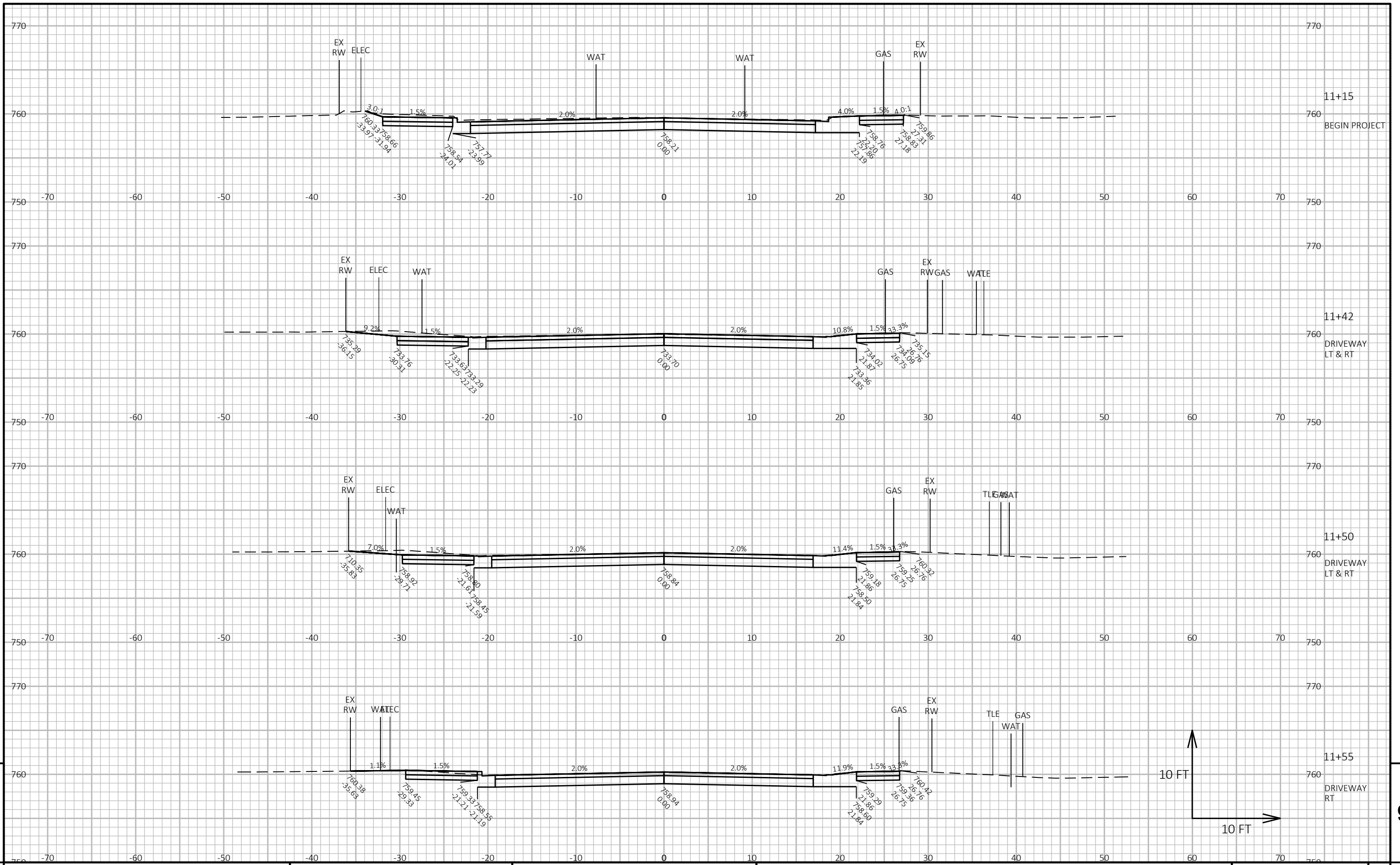
LEGEND

- A.S.T./C.S.M. - ARCHITECTURAL SURFACE TREATMENT/CONCRETE STAINING MULTI-COLOR.
- ⊕ DO NOT PLACE FILL WITHIN 1'-6" OF WING SURFACES TO BE STAINED UNTIL AFTER C.S.M. IS COMPLETE.
- * TAPER PROPOSED GRADE FROM TOP OF SIDEWALK TO TOP OF WINGS AS SHOWN AND AS DIRECTED BY ENGINEER.

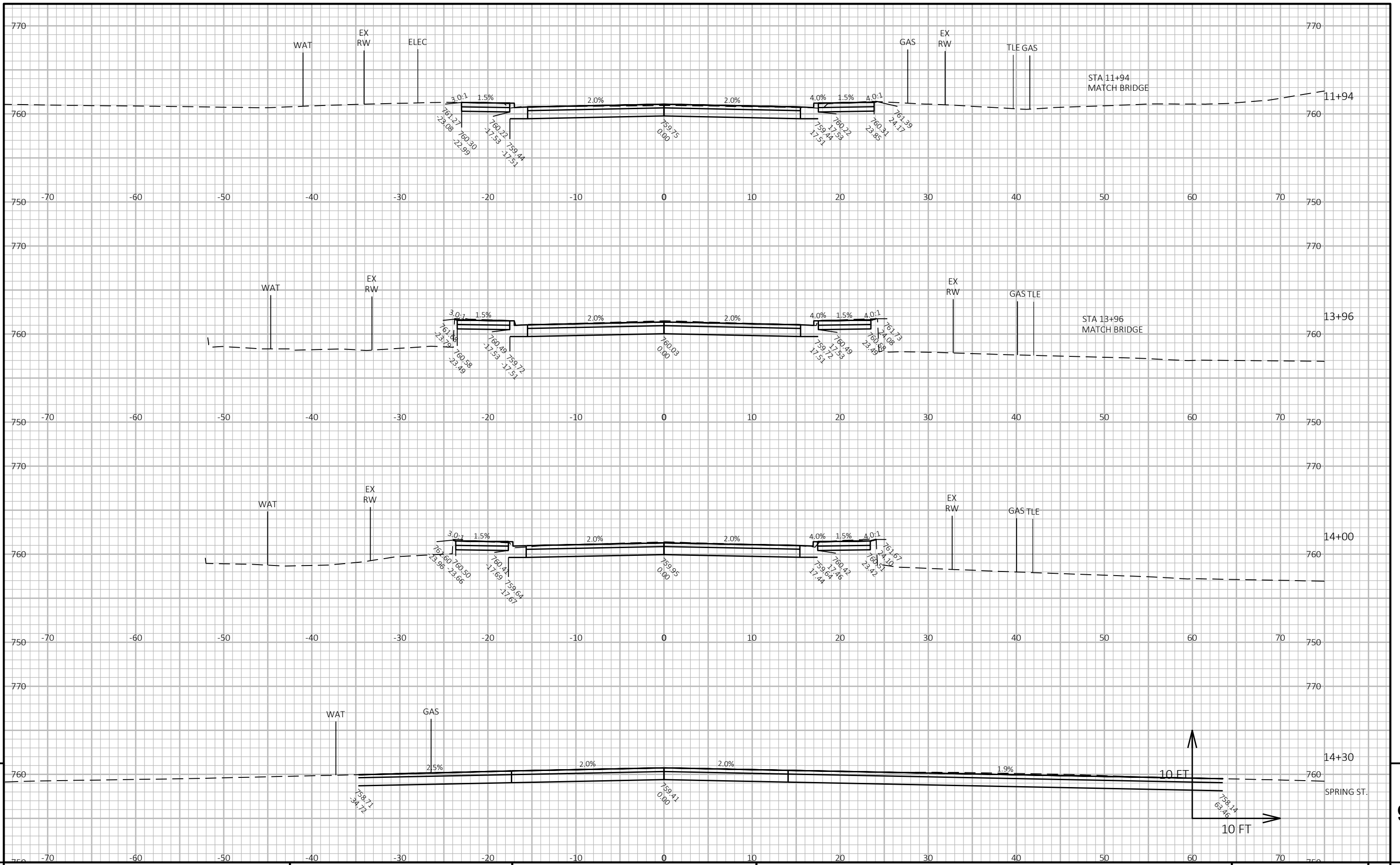
NO.	DATE	REVISION	BY
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION			
STRUCTURE B-51-154			
DRAWN BY		PKE	PLANS CK'D. BH
AESTHETIC DETAILS			SHEET 28 OF 28



STATION	Real Station	Distance	AREA (SF)		Incremental Vol (CY) (Unadjusted)		Cumulative Vol (CY)		Mass Ordinate Note 8
			Cut	Fill	Cut Note 1	Fill Note 3	Cut 1.00 Note 1	Expanded Fill 1.25	
11+15	1115.00	0.00	83.51	0.00	0	0	0	0	0.00
11+42	1142.00	27.00	77.94	0.00	81	0	81	0	66.98
11+50	1150.00	8.00	76.42	0.00	23	0	104	0	85.77
11+55	1155.00	5.00	70.56	0.00	14	0	117	0	96.83
11+94	1194.00	39.00	55.17	0.00	91	0	208	0	167.78
13+96	1396.00	0.00	62.20	0.00	0	0	208	0	167.78
14+00	1400.00	4.00	60.04	0.00	9	0	217	0	174.79
14+30	1430.00	30.00	133.23	0.00	107	0	324	0	266.89
14+50	1450.00	20.00	54.86	1.28	70	0	394	0	325.97
14+95	1495.00	45.00	64.80	0.98	100	2	494	3	400.42



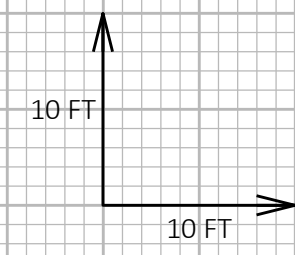
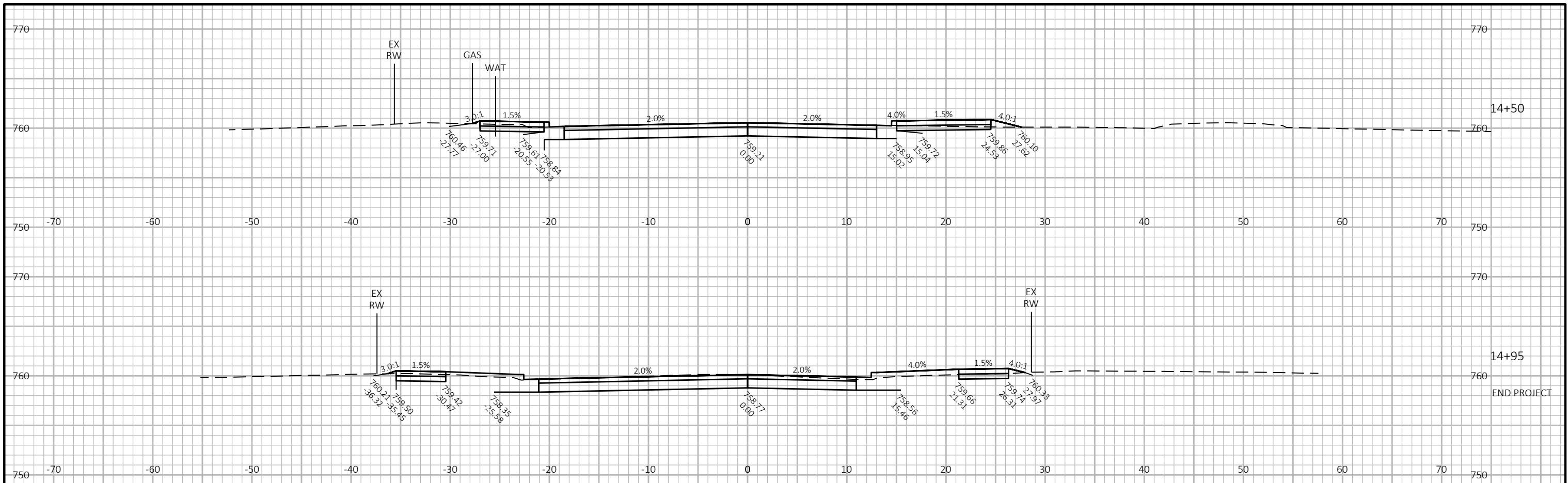
PROJECT NO: 3834-00-72 HWY: JEFFERSON STREET COUNTY: RACINE CROSS SECTIONS: JEFFERSON STREET SHEET 9



PROJECT NO: 3834-00-72	HWY: JEFFERSON STREET	COUNTY: RACINE	CROSS SECTIONS: JEFFERSON STREET	SHEET
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PROJECT NO: 3834-00-72	HWY: JEFFERSON STREET	COUNTY: RACINE	CROSS SECTIONS: JEFFERSON STREET	SHEET E
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Wisconsin Department of Transportation

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