

WKE PROJECT ID: 3840-06-71

Nov 8, 2022
ORDER OF SHEETS

Section No.	Title
1	1
2	2
3	3
3	3
4	Right of Way Plat
5	5
6	6
7	7
8	8
9	Computer Earthwork Data
9	Cross Sections

TOTAL SHEETS = 124

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

C ELKHORN, MARKET ST STH 11 TO STH 67 LOCAL STREET WALWORTH COUNTY

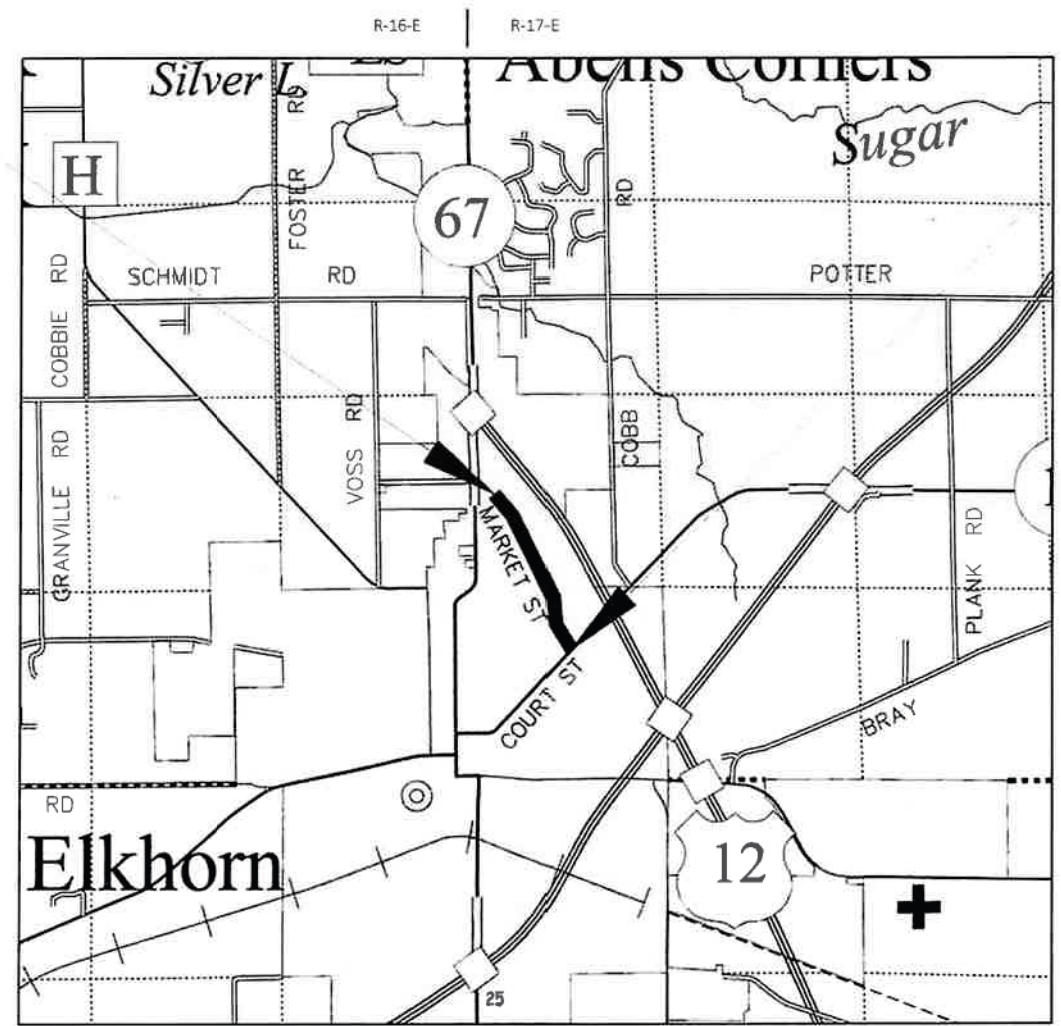
STATE PROJECT NUMBER
3840-06-71

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
3840-06-71	WISC 2023035	1



BEGIN PROJECT
STA 203+50.00
Y=372,579.767
X=763,354.150

END PROJECT
STA 254+15.00



DESIGN DESIGNATION

A.A.D.T.	2019	=	1,800
A.A.D.T.	2043	=	2,240
D.H.V.		=	216
D.D.		=	50/50
T.		=	5%
DESIGN SPEED		=	35 MPH
ESALS		=	595,668

CONVENTIONAL SYMBOLS

PLAN	PROFILE
CORPORATE LIMITS	GRADE LINE
PROPERTY LINE	ORIGINAL GROUND
LOT LINE	MARSH OR ROCK PROFILE (To be noted as such)
LIMITED HIGHWAY EASEMENT	SPECIAL DITCH
EXISTING RIGHT OF WAY	GRADE ELEVATION
PROPOSED OR NEW R/W LINE	CULVERT (Profile View)
SLOPE INTERCEPT	UTILITIES
REFERENCE LINE	ELECTRIC
EXISTING CULVERT	FIBER OPTIC
PROPOSED CULVERT (Box or Pipe)	GAS
COMBUSTIBLE FLUIDS	SANITARY SEWER
MARSH AREA	STORM SEWER
WOODED OR SHRUB AREA	TELEPHONE
	WATER
	UTILITY PEDESTAL
	POWER POLE
	TELEPHONE POLE



TOTAL NET LENGTH OF CENTERLINE = 0.959 MI

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS), WALWORTH COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES.

ELEVATIONS ARE REFERENCED TO NAVD 88 (2012). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 12A.

ACCEPTED FOR
CITY OF ELKHORN
Date: 07/13/22
MATTHEW LINDSTROM
OPERATIONS MANAGER
DEPARTMENT OF PUBLIC WORKS

ORIGINAL PLANS PREPARED BY



DATE: 7-13-22
(Professional Engineer Signature)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY

Surveyor	KAPUR & ASSOCIATES
Designer	KAPUR & ASSOCIATES
Project Manager	MICHAEL BAIRD
Regional Examiner	
Regional Supervisor	JEFF BOHEN

APPROVED FOR THE DEPARTMENT
DATE: 7/18/22
Michael Baird
(Signature)

E

GENERAL NOTES

STANDARD ABBREVIATIONS

2

NO TREES OR SHRUBS SHALL BE REMOVED WITHOUT APPROVAL OF THE ENGINEER.

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. EXISTING UTILITIES ARE SHOWN FROM AS-BUILT PLANS PROVIDED BY THE UTILITY. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

THE CONTRACTOR MUST NOTIFY DIGGERS HOTLINE AND AFFECTED UTILITIES PRIOR TO THE START OF WORK. ANY LOCAL MUNICIPAL UTILITY WHICH IS NOT A MEMBER OF THE DIGGERS HOTLINE MUST BE CONTACTED SEPARATELY.

EROSION CONTROL DEVICES SHALL BE PLACED IN SEQUENCE WITH CONSTRUCTION OPERATIONS OR AS DETERMINED BY THE ENGINEER.

THE EROSION CONTROL FEATURES AS SHOWN IN THE PLANS ARE AT SUGGESTED LOCATIONS. EXACT LOCATIONS WILL BE DETERMINED BY THE CONTRACTOR'S ECIP AND ENGINEER IN THE FIELD.

ALL PRIVATE EXISTING UTILITIES ARE TO BE ADJUSTED BY THE UTILITIES CONCERNED. SANITARY SEWER MANHOLES AND WATER VALVE BOXES, WILL BE ADJUSTED AND PAID FOR AS BID ITEMS.

THE EXACT LOCATION OF EXCAVATION BELOW SUBGRADE (EBS) WILL BE DETERMINED BY THE ENGINEER.

BROKEN CONCRETE CONTAINING RE-BAR SHALL NOT BE USED AS RIPRAP OR EBS BACKFILL.

CONCRETE CURB AND GUTTER GRADES ARE TO THE FLANGE OF CURB AND GUTTER. CURB AND GUTTER RADII ARE MEASURED TO THE FLANGE OF CURB.

THE LOCATION OF DRIVEWAYS IS TO BE DETERMINED BY THE ENGINEER AND REPLACED IN KIND UNLESS NOTED OTHERWISE.

CONCRETE DRIVEWAY REMOVAL WILL BE PAID FOR AS REMOVING CONCRETE PAVEMENT.

REMOVAL OF EROSION CONTROL DEVICES WILL BE INCLUDED IN THE COST OF THE RESPECTIVE BID ITEMS.

TRAFFIC CONTROL DEVICES WILL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

CONTRACTOR MUST CONTACT PROJECT ENGINEER AND SEWRPC AT LEAST TWO WEEKS PRIOR TO CONDUCTING WORK NEAR ANY PUBLIC SURVEY MONUMENT.

A SAWED JOINT IS REQUIRED WHERE NEW PAVEMENT IS TO MEET AN EXISTING PAVED SURFACE.

PAVING OPERATIONS SHALL BE CONSISTENT WITH THE TYPICAL SECTIONS, WITH LONGITUDINAL JOINTS CONSTRUCTED AT LANE LINES ONLY.

LOCATIONS OF SIGNS SHOWN IN THE PLANS ARE APPROXIMATE AND THE FINAL LOCATION OF SIGNS ARE TO BE DETERMINED BY THE ENGINEER.

TOPSOIL SHALL BE REPLACED WITH A 4-INCH TYPICAL DEPTH.

SIGNS IN CONFLICT WITH TRAFFIC CONTROL "IN USE" WILL BE COVERED AS DIRECTED BY THE ENGINEER AND PAID FOR UNDER ITEM "TRAFFIC CONTROL COVERING SIGNS TYPE II".

SQUEEGEE SAWCUT SLURRY TO CURB HEAD AND REMOVE BEFORE ADVANCING TO NEXT CUT.

HMA PAVEMENT SHALL BE CONSTRUCTED WITH THE FOLLOWING LAYERS :

4-INCH HMA PAVEMENT
 1 3/4" UPPER LAYER 4 MT 58-28 S
 2 1/4" LOWER LAYER 3 MT 58-28 S

2

AEW	APRON ENDWALL	SY	SQUARE YARD
AGG	AGGREGATE	T	TANGENT LENGTH
BAD	BASE AGGREGATE DENSE	TLE	TEMPORARY LIMITED EASEMENT
BM	BENCHMARK	VCL	VERTICAL CURVE LENGTH
BTWN	BETWEEN	VPC	POINT OF VERTICAL CURVE
C&G	CURB AND GUTTER	VPI	POINT OF VERTICAL INTERSECTION
C/L	CENTER OR CONSTRUCTION LINE	VPT	POINT OF VERTICAL TANGENT
CMCP	CULVERT PIPE CORRUGATED METAL		
CONC	CONCRETE		
CP	CULVERT PIPE		
CPRC	CULVERT PIPE REINFORCED CONCRETE		
CSD	CONCRETE SURFACE DRAIN		
CY	CUBIC-YARD		
D	DEGREE OF CURVE		
Δ	DELTA		
DISCH	DISCHARGE		
FE	FIELD ENTRANCE		
HMA	HOT MIX ASPHALT		
INV	INVERT		
L	LENGTH OF CURVE		
LHF	LEFT HAND FORWARD		
LT	LEFT		
MIN	MINIMUM		
M/L	MATCHLINE		
NB	NORTHBOUND		
NC	NORMAL CROWN		
PAVT	PAVEMENT		
PC	POINT OF CURVE		
PCC	POINT OF COMPOUND CURVE		
PE	PRIVATE ENTRANCE		
PI	POINT OF INTERSECTION		
PLE	PERMANENT LIMITED EASEMENT		
PT	POINT OF TANGENT		
R	RADIUS OF CURVE		
R/L	REFERENCE LINE		
R/W	RIGHT OF WAY		
RC	REVERSE CROWN		
RCAEW	APRON ENDWALL FOR CULVERT PIPE REINFORCED CONCRETE		
REQD	REQUIRED		
RHF	RIGHT HAND FORWARD		
RO	RUN OFF LENGTH		
RRSP	RAILROAD SPIKE		
RT	RIGHT		
SALV	SALVAGED		
SAPBC	SALVAGED ASPHALTIC PAVEMENT BASE COURSE		
SB	SOUTHBOUND		
SDD	STANDARD DETAIL DRAWING		
SE	SUPERELEVATION		
SF	SQUARE FOOT		
STA	STATION		

PROJECT NO: 3840-06-71

HWY: MARKET STREET

COUNTY: WALWORTH

GENERAL NOTES

SHEET

E

UTILITY CONTACTS

CHARTER COMMUNICATIONS - COMMUNICATION LINE

CODY THOMPSON
510 BELOIT STREET
WALWORTH, WI 53121
EMAIL: CODY.THOMPSON@CHARTER.COM
EMAIL: WIS.ENGINEERING@CHARTER.COM

ELKHORN, CITY OF - DPW

MATT LINDSTROM
PUBLIC WORKS OPERATIONS MANAGER
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PHONE: (262) 723-2223
EMAIL: MLINDSTROM@CITYOFELKHORN.ORG

ELKHORN, CITY OF- ELECTRIC

BZ KAYSER
400 KOOPMAN LANE
ELKHORN, WI 53121
PHONE: (262) 723-3138
EMAIL: QKAYSER@CITYOFELKHORN.ORG

ELKHORN, CITY OF- WATER

TIM BOSS
400 KOOPMAN LANE
ELKHORN, WI 53121
PHONE: (262) 723-3138
EMAIL: TBOSS@CITYOFELKHORN.ORG

TDS TELECOM

JASON KENNY
525 JUNCTION RD
MADISON, WI 53717
PHONE: (262) 514-2127
EMAIL: JASON.KENNY@TDSTELECOM.COM

WE ENERGIES - GAS

SCOTT HOLSTEIN
700 S. KANE STREET
BURLINGTON, WI 53105
PHONE: (262) 763-1084
EMAIL: WE-UTILITY-RELOCATIONS@WE-ENERGIES.COM

WE ENERGIES - GAS EMERGENCY
PHONE: (800) 261-5325

STATE AGENCIES

WISCONSIN DEPT. OF NATURAL RESOURCES

CRAIG WEBSTER
141 NW BARSTOW STREET, ROOM 180
WAUKESHA, WI 53188
PHONE: (262) 574-2141
EMAIL: CRAIG.WEBSTER@WISCONSIN.GOV

WISDOT - PROJECT MANAGER

MICHAEL BAIRD
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WAUKESHA, WI 53188
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OTHER CONTACTS

KAPUR & ASSOCIATES

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7711 N. PORT WASHINGTON ROAD
MILWAUKEE, WI 53217
PHONE: (414) 751-7230
EMAIL: ABUBB@KAPURINC.COM

ELKHORN FIRE RESCUE

ROD SMITH
13 S. BROAD STREET
ELKHORN, WI 53121
PHONE: (262) 723-2277
EMAIL: RSMITH@CITYOFELKHORN.ORG

ELKHORN POLICE DEPARTMENT

JOEL CHRISTENSEN
100 W. WALWORTH STREET
ELKHORN, WI 53121
PHONE: (262) 723-2210
EMAIL: INFO@ELKHORNPD.ORG

SOUTHEASTERN WISCONSIN REGIONAL PLANNING COMMISSION

ROB MERRY
W239 N1812 ROCKWOOD DRIVE
P.O. BOX 1607
WAUKESHA, WI 53187-1607
PHONE: (262) 953-4289
EMAIL: RMERRY@SEWRPC.ORG

ORDER OF SECTION 2 SHEETS

- GENERAL NOTES
- PROJECT OVERVIEW
- TYPICAL SECTIONS
- CONSTRUCTION DETAILS
- PLAN DETAILS
- CURB RAMP DETAILS
- EXISTING UTILITY PLAN
- SIGNING AND MARKING
- DETOUR PLAN
- PEDESTRIAN STAGING PLAN
- ALIGNMENT PLAN

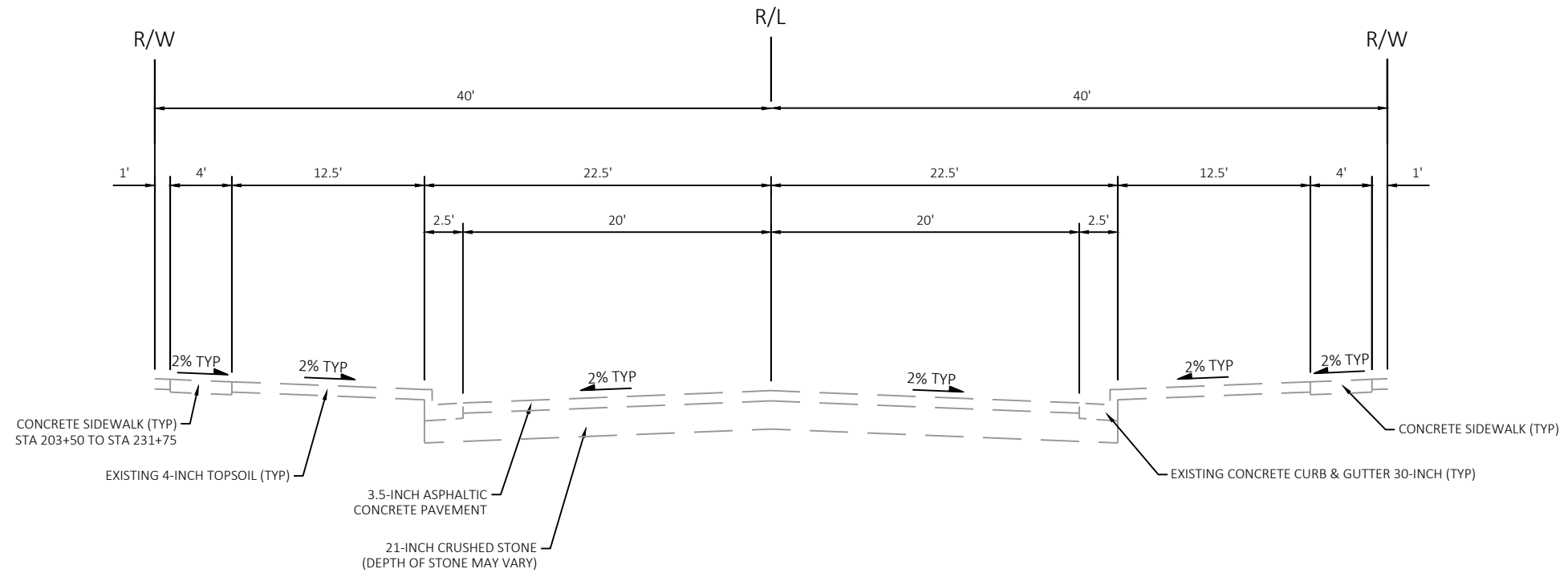




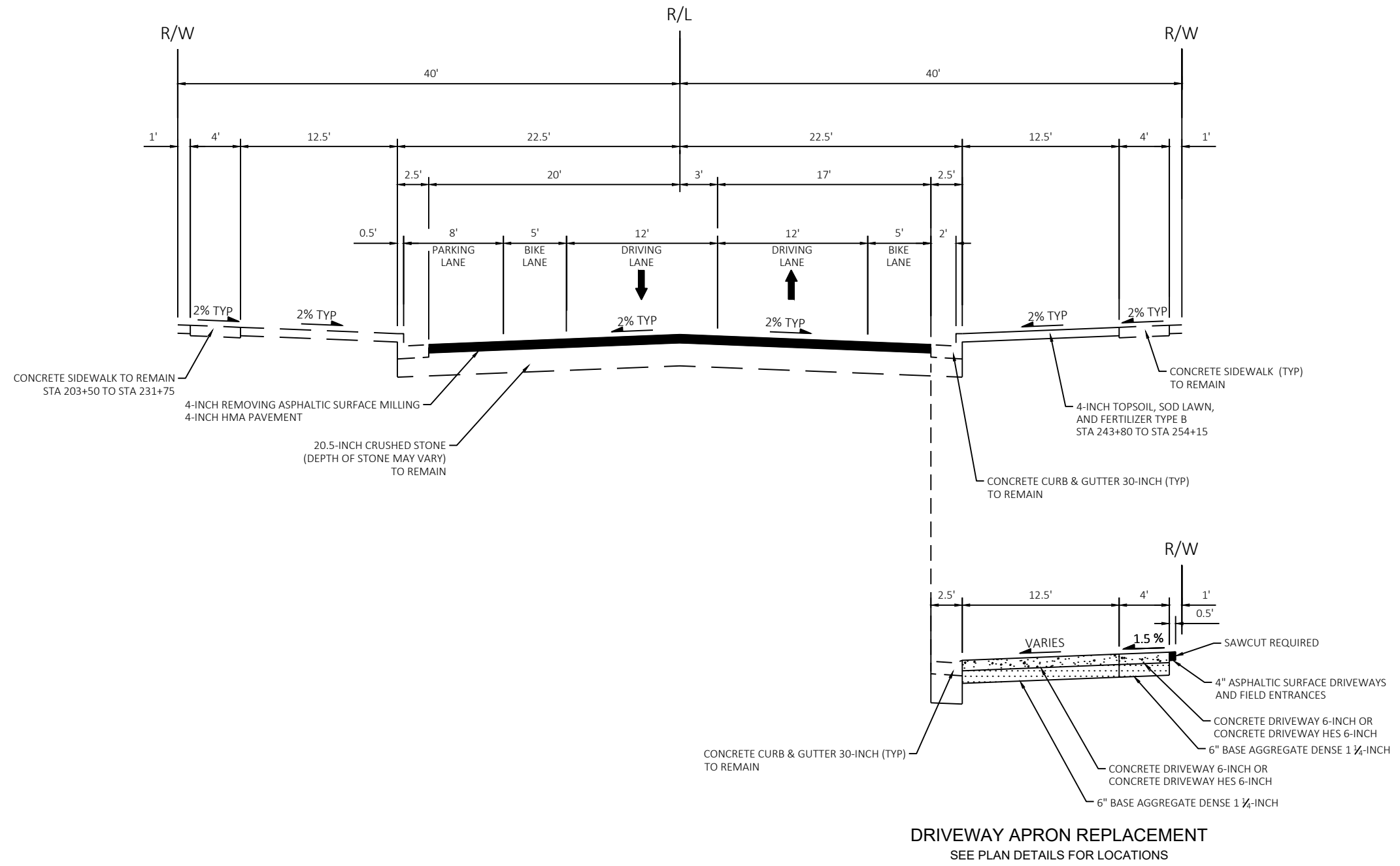
2

2

PROJECT NO: 3840-06-71	HWY: MARKET STREET	COUNTY: WALWORTH	PROJECT OVERVIEW	SHEET	E
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MARKET STREET
EXISTING TYPICAL SECTION
 STA 203+50 TO STA 254+15



**MARKET STREET
PROPOSED TYPICAL SECTION
STA 203+50 TO STA 254+15**

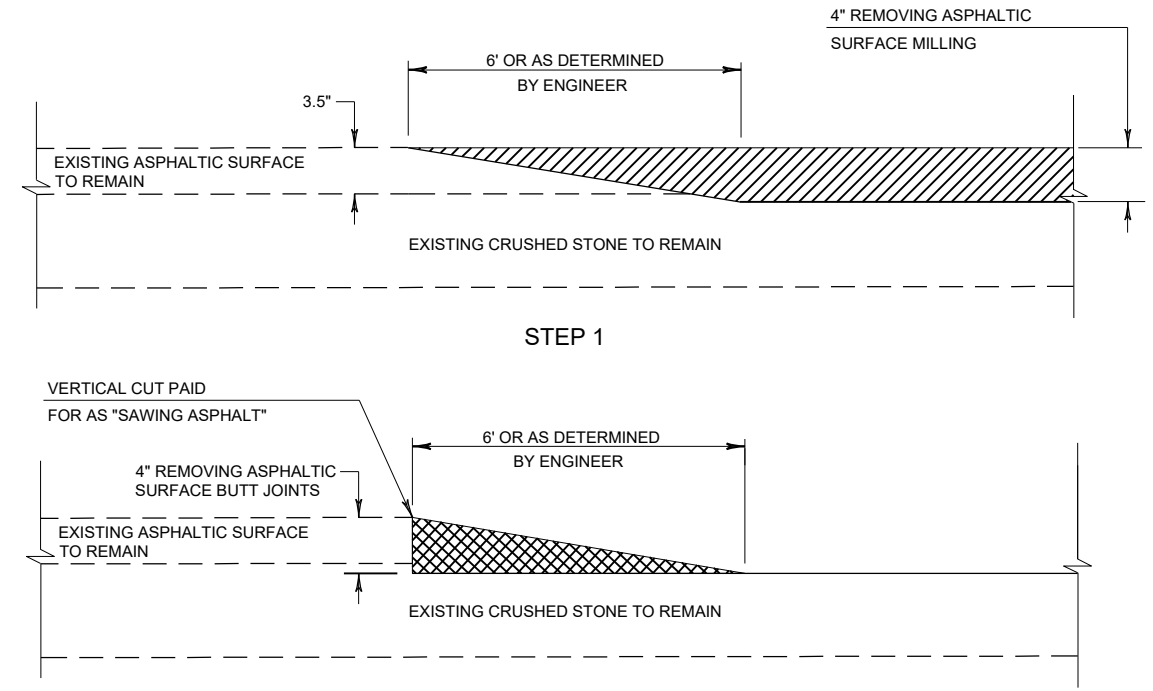
**DRIVEWAY APRON REPLACEMENT
SEE PLAN DETAILS FOR LOCATIONS**



FIXED MESSAGE SIGN

NOTES

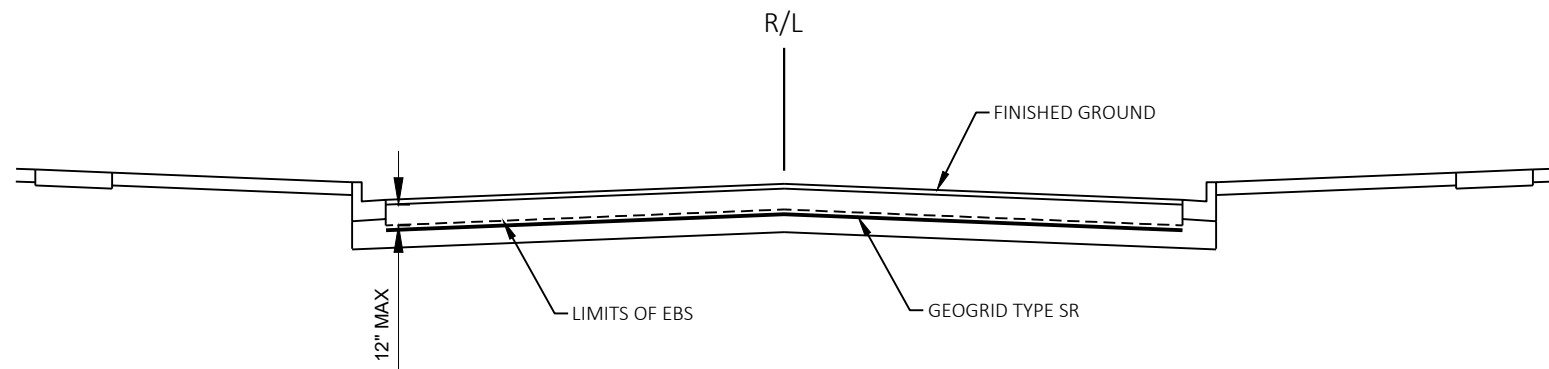
- 1) SEE DETOUR PLAN FOR FIXED MESSAGE SIGN LOCATIONS.
- 2) SIGNS SHALL HAVE BLACK NON-REFLECTIVE MESSAGE ON ORANGE FLUORESCENT REFLECTIVE BACKGROUND. SHEETING AND BASE MATERIAL PER SPEC 637 & 643. REFERENCE: "WISDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION," LATEST EDITION.
- 3) SIGNS SHALL HAVE 6" CAPITAL LETTERS, SERIES "C".
- 4) SIGNS SHALL HAVE A 1" BLACK BORDER WITH 3" CORNER RADIUS.
- 5) CONTRACTOR SHALL SUBMIT SHOP DRAWINGS TO ENGINEER FOR REVIEW PRIOR TO MANUFACTURING.
- 6) SIGNS SHALL BE MOUNTED ON 4"x6" WOODEN POST SUPPORTS, WITH 2 HOLES PER STATE PLATE A4-11. THE NUMBER OF POSTS REQUIRED IS SHOWN.



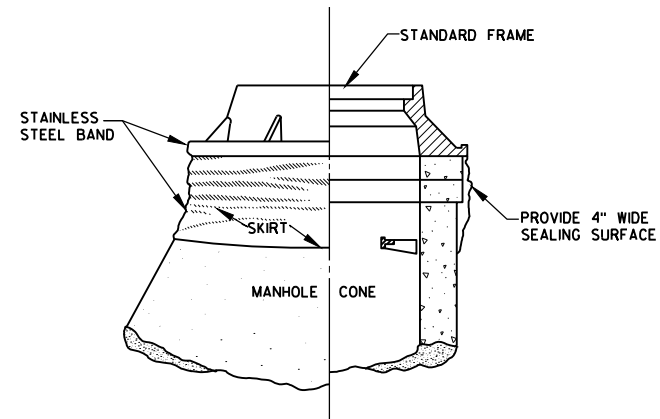
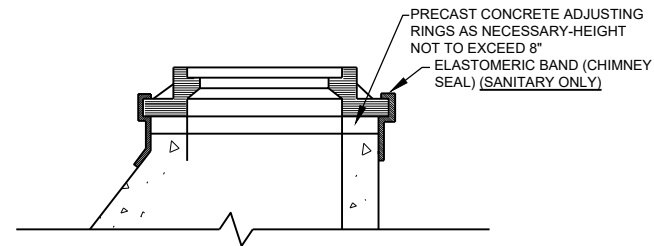
MILLING DETAIL - BUTT JOINTS

NOTES

- 1) LIMITS TO BE DETERMINED BY ENGINEER IN FIELD.
- 2) BACKFILL USING BASE AGGREGATE DENSE 1-1/4 INCH AS DIRECTED BY THE ENGINEER.



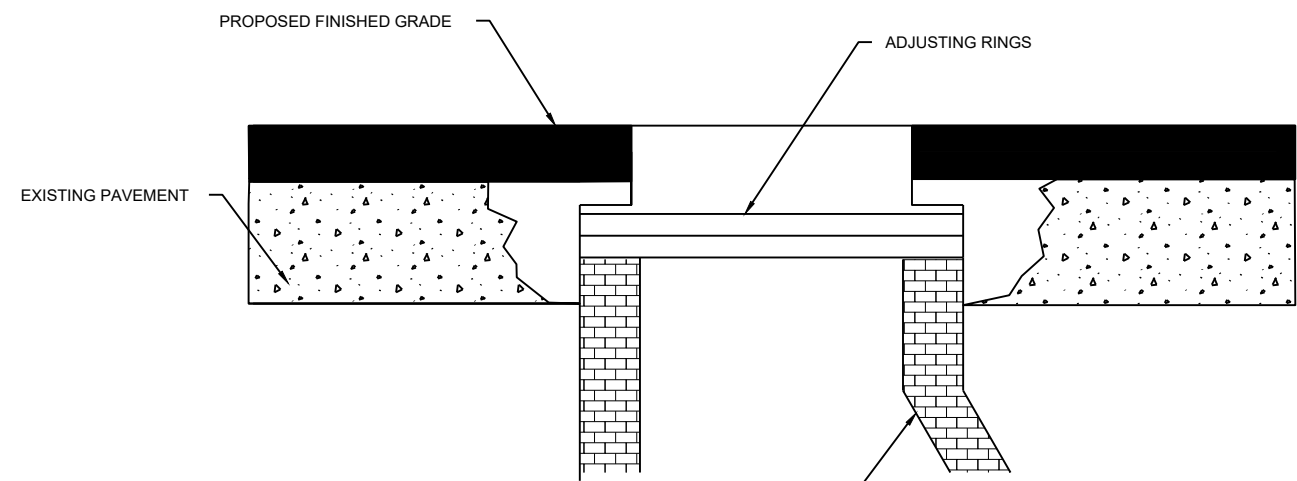
EXCAVATION BELOW SUBGRADE (EBS)



1. CHIMNEY SEALS SHALL BE INSTALLED ON ALL SANITARY SEWERAGE SYSTEM MANHOLES.
2. INTERNAL / EXTERNAL SEALS BY ADAPTER INC OR EXTERNAL SEALS BY CRETEX ARE RECOMMENDED; ALL OTHER PRODUCTS OR OTHER DESIGN SOLUTIONS SHALL REQUIRE THE APPROVAL OF THE CITY ENGINEER.
3. CHIMNEY SEALS SHALL BE INSTALLED IN ACCORDANCE WITH THE MANUFACTURER'S INSTRUCTIONS.

ADJUSTING SANITARY MANHOLE

NOTE
 ADJUST CHIMNEY WITH PROPER SIZE CONCRETE ADJUSTING RINGS (SIZE OF OPENING OF CONCRETE ADJUSTING RING TO BE DETERMINED BY SIZE OF OPENING IN FRAME) AND REINSTALL EXISTING FRAME AND LID TO FINISHED GRADE. (BACKFILL AND OTHER WORK TO BE DONE IN ACCORDANCE WITH ITEM ADJUSTING INLET COVERS.)





REMOVE AND REPLACE DAMAGED OR LOOSE BRICK OR BLOCK AS DIRECTED BY ENGINEER IN THE FIELD.








**PAVED AREA
 ADJUSTING MANHOLE OR INLET COVERS**



EROSION CONTROL LEGEND

-  SLOPE INTERCEPT
-  INLET PROTECTION AND TYPE

PLAN DETAIL LEGEND

-  ASPHALTIC SURFACE MILLING 4" REPLACE WITH 4" HMA PAVEMENT
-  CONCRETE CURB AND GUTTER, 30-INCH TYPE D
-  CONCRETE SIDEWALK 5-INCH
-  CONCRETE DRIVEWAY 6-INCH
-  CONCRETE DRIVEWAY HES 6-INCH
-  ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES, 3-INCH
-  4-INCH TOPSOIL, SOD LAWN, AND FERTILIZER TYPE B
- YYYYYYY SAWING ASPHALT
- XXXXXXX SAWING CONCRETE

NOTES:

1) SEE CURB RAMP DETAIL SHEETS FOR ADDITIONAL INFORMATION.



BEGIN PROJECT
STA 203+50.00

R/L MARKET ST

ADJUST MANHOLE
REPLACE DAMAGED RINGS
ASSUME 2"-4"

MARKET ST

202

203

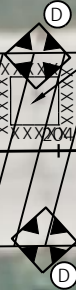
204

205

206

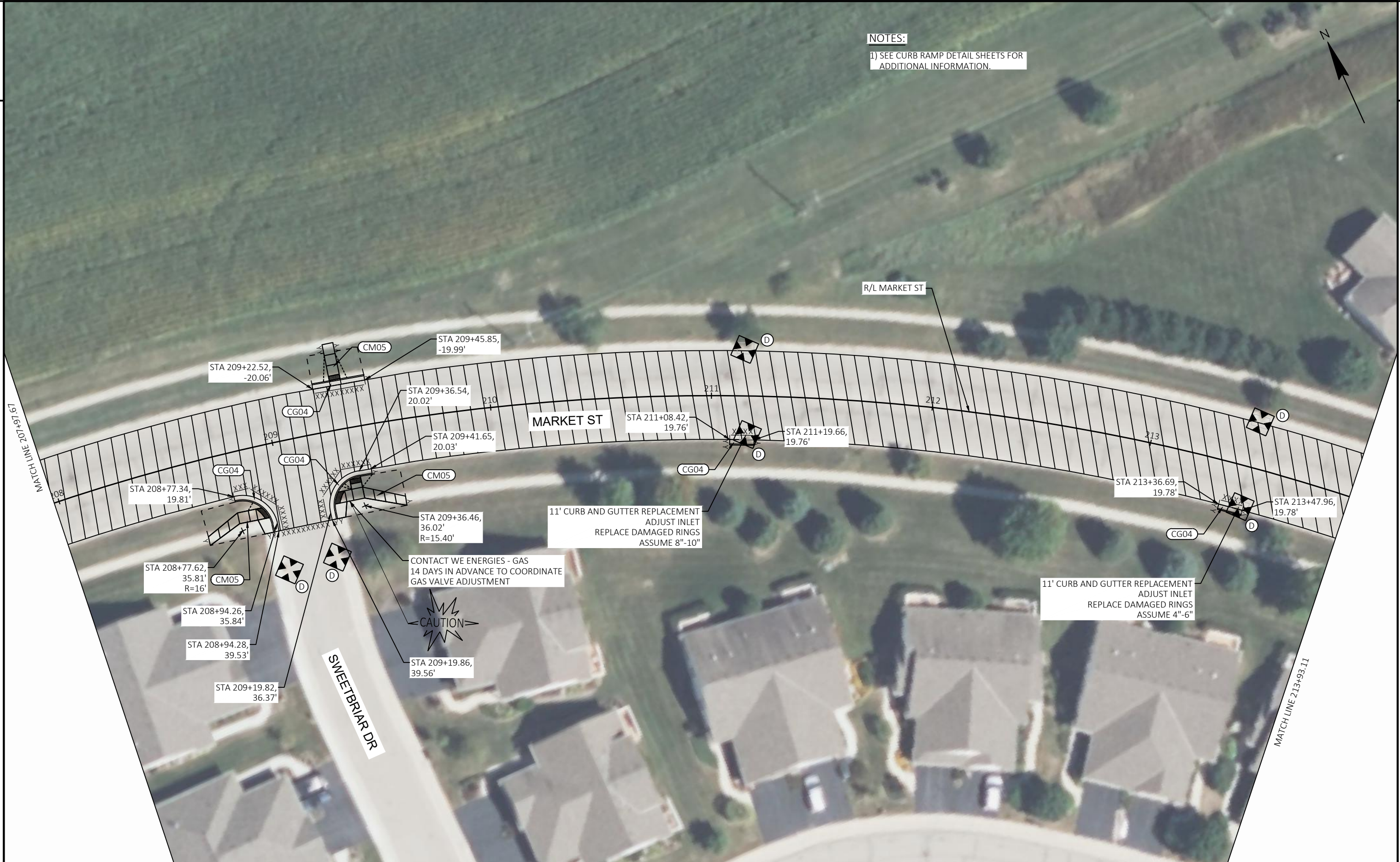
207

MATCH LINE 207+97.67



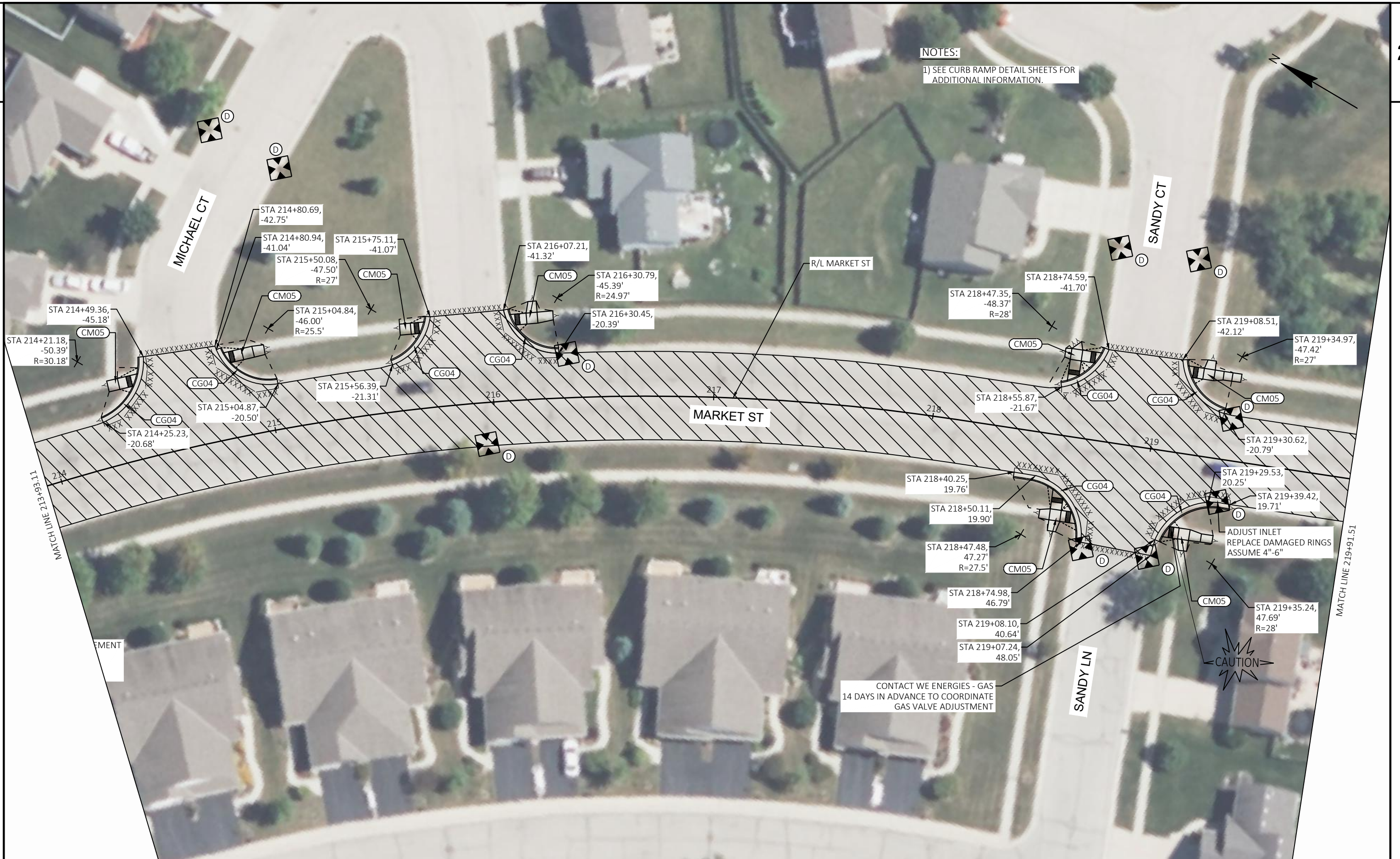
NOTES:

1) SEE CURB RAMP DETAIL SHEETS FOR ADDITIONAL INFORMATION.



NOTES:

1) SEE CURB RAMP DETAIL SHEETS FOR ADDITIONAL INFORMATION.



PROJECT NO: 3840-06-71

HWY: MARKET STREET

COUNTY: WALWORTH

PLAN DETAILS

SHEET

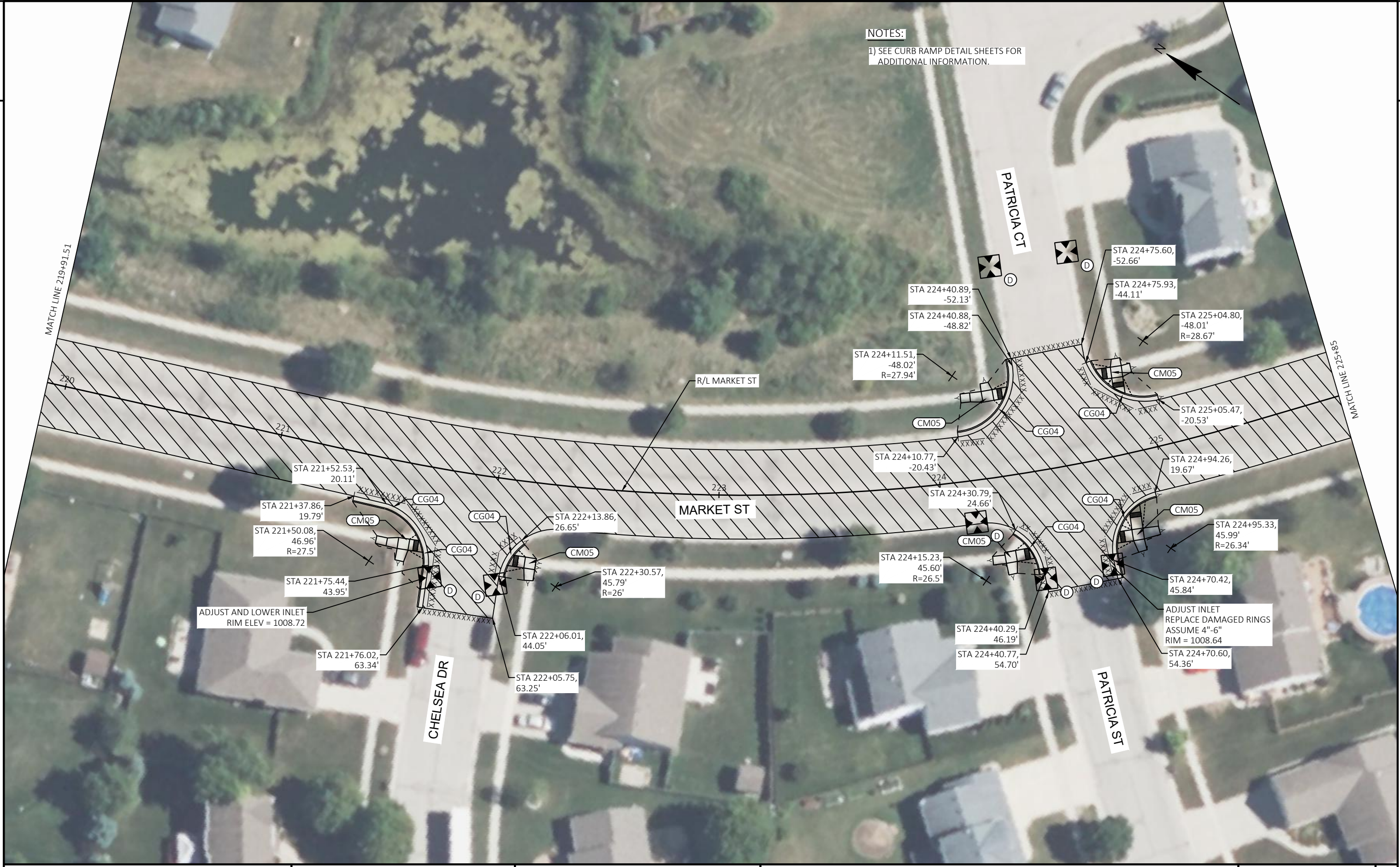
E

NOTES:

1) SEE CURB RAMP DETAIL SHEETS FOR ADDITIONAL INFORMATION.

MATCH LINE 219+91.51

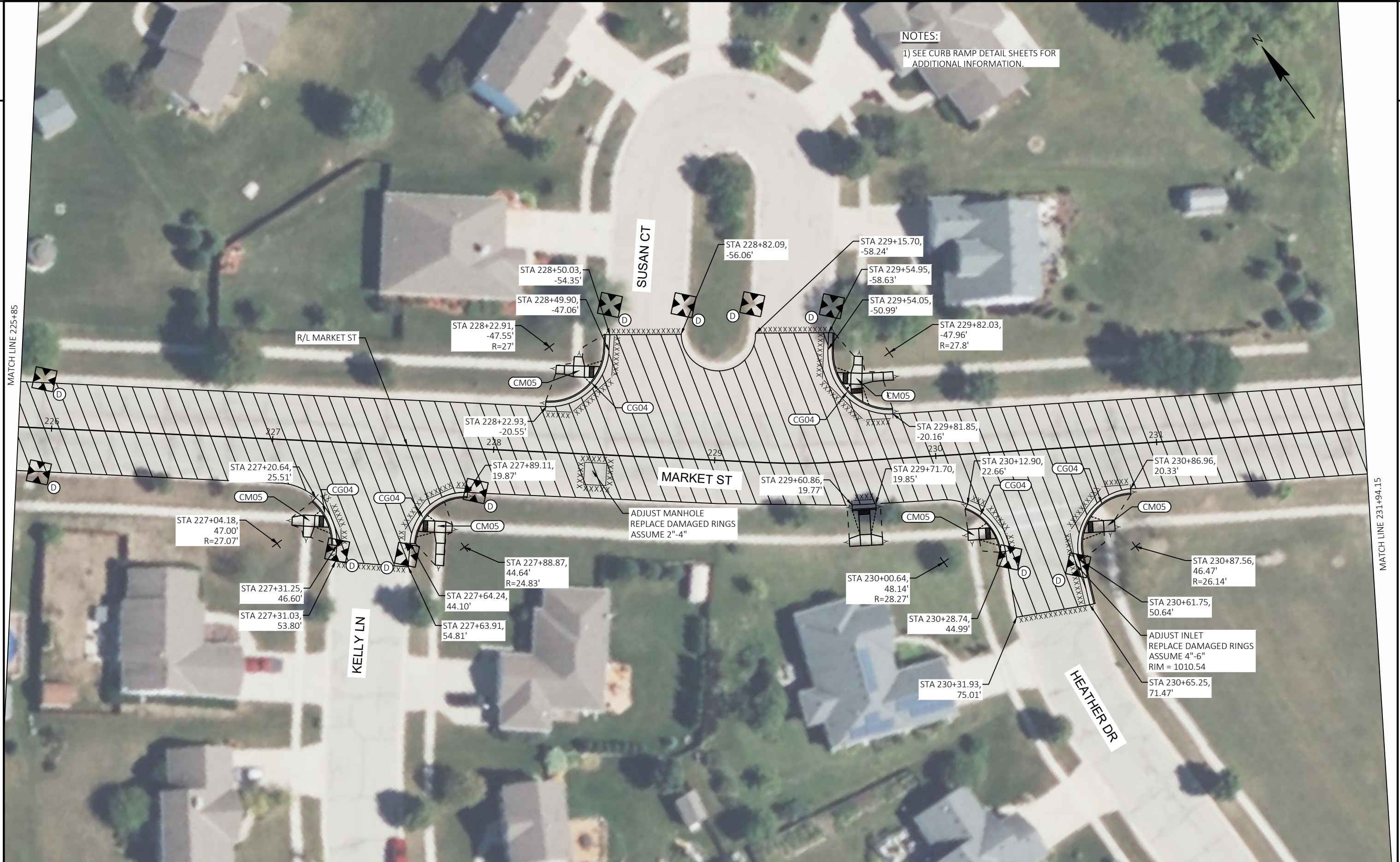
MATCH LINE 225+85



PROJECT NO: 3840-06-71	HWY: MARKET STREET	COUNTY: WALWORTH	PLAN DETAILS	SHEET	E
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NOTES:

1) SEE CURB RAMP DETAIL SHEETS FOR ADDITIONAL INFORMATION.



NOTES:

1) SEE CURB RAMP DETAIL SHEETS FOR ADDITIONAL INFORMATION.



R/L MARKET ST

MARKET ST

DYLAN DR

MATCH LINE 231+94.15

MATCH LINE 237+92.08

232

233

234

235

236

237

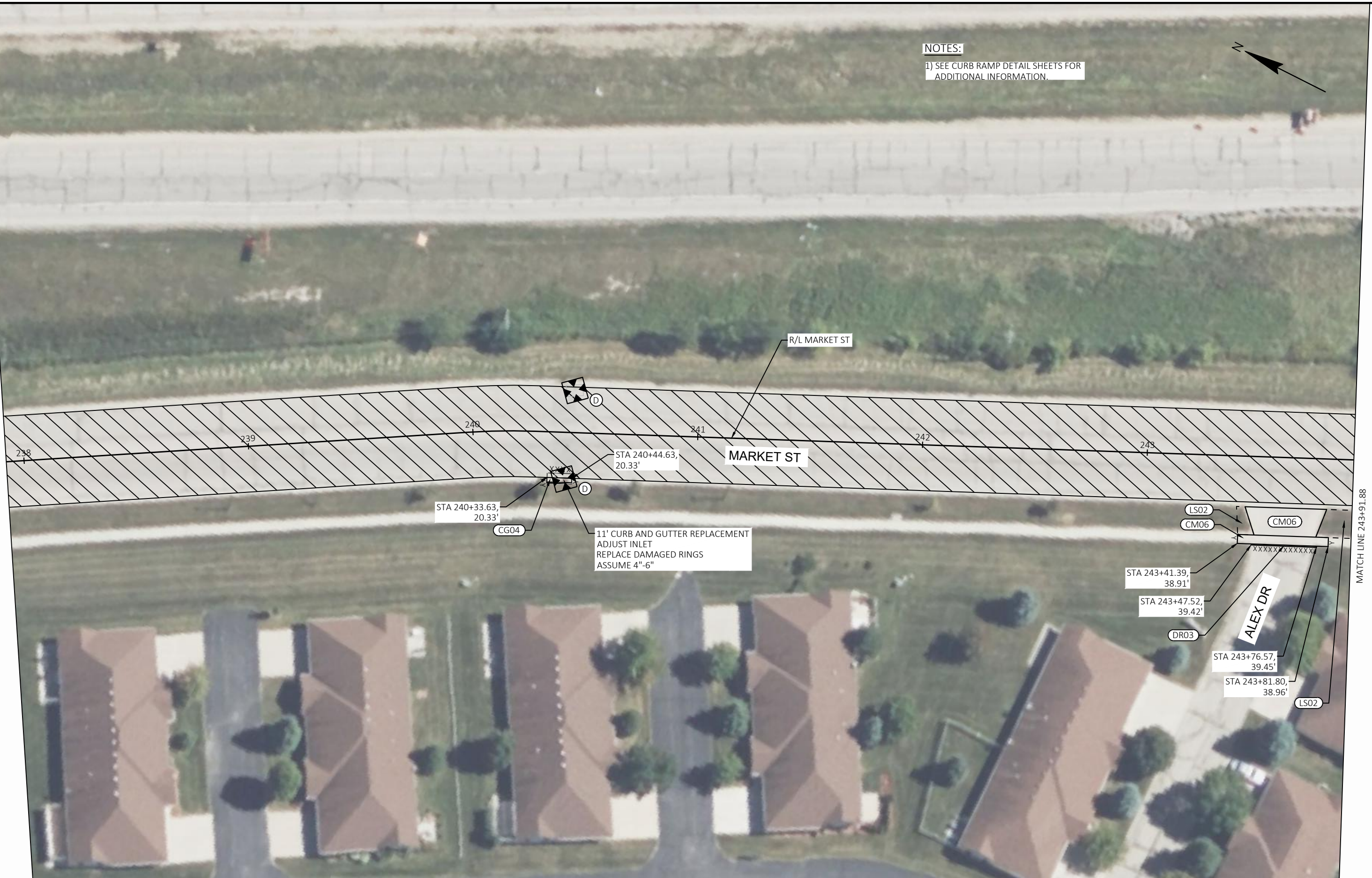
NOTES:

1) SEE CURB RAMP DETAIL SHEETS FOR ADDITIONAL INFORMATION.



MATCH LINE 237+92.08

MATCH LINE 243+91.88



PROJECT NO: 3840-06-71

HWY: MARKET STREET

COUNTY: WALWORTH

PLAN DETAILS

SHEET

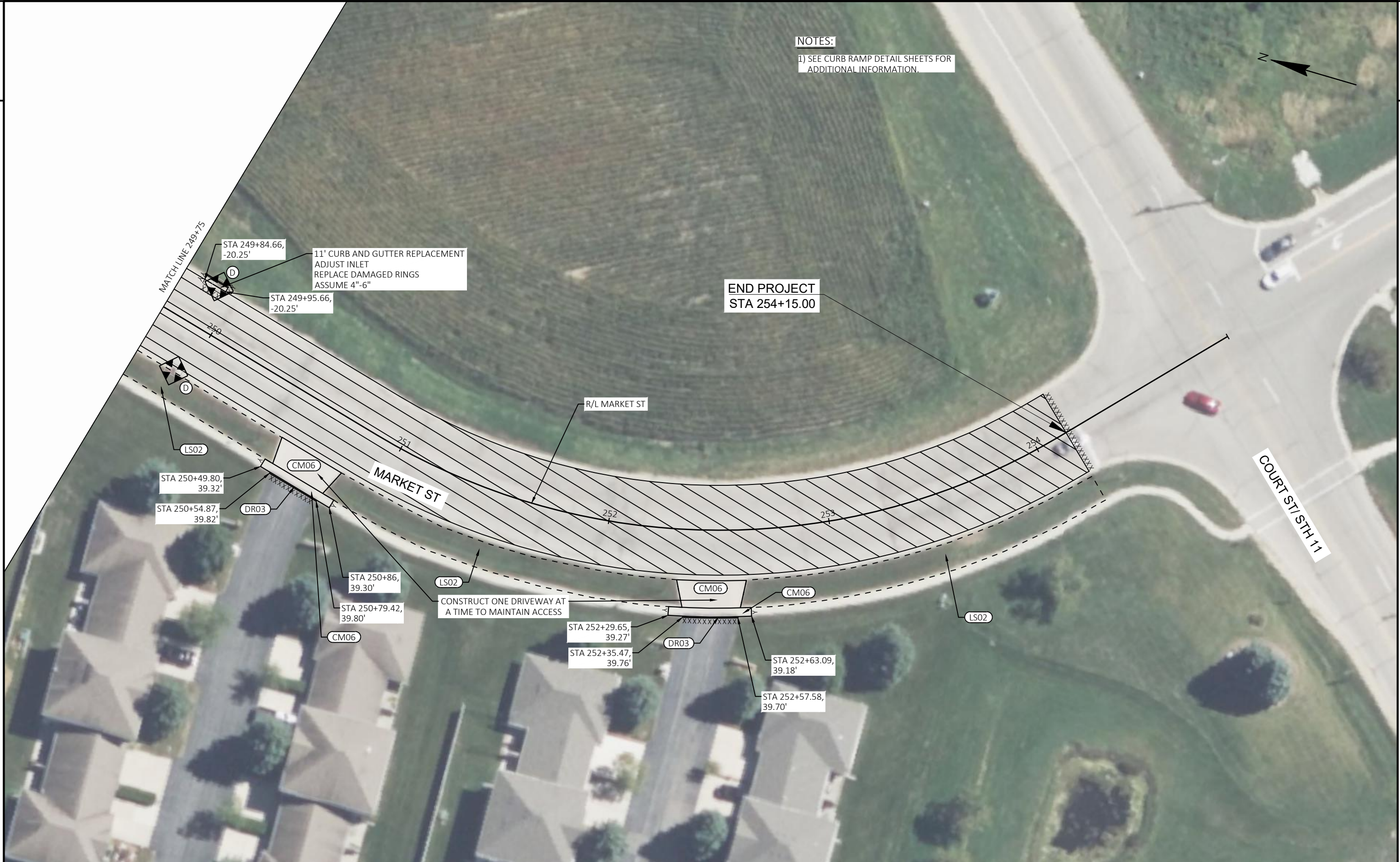
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NOTES:
1) SEE CURB RAMP DETAIL SHEETS FOR
ADDITIONAL INFORMATION.



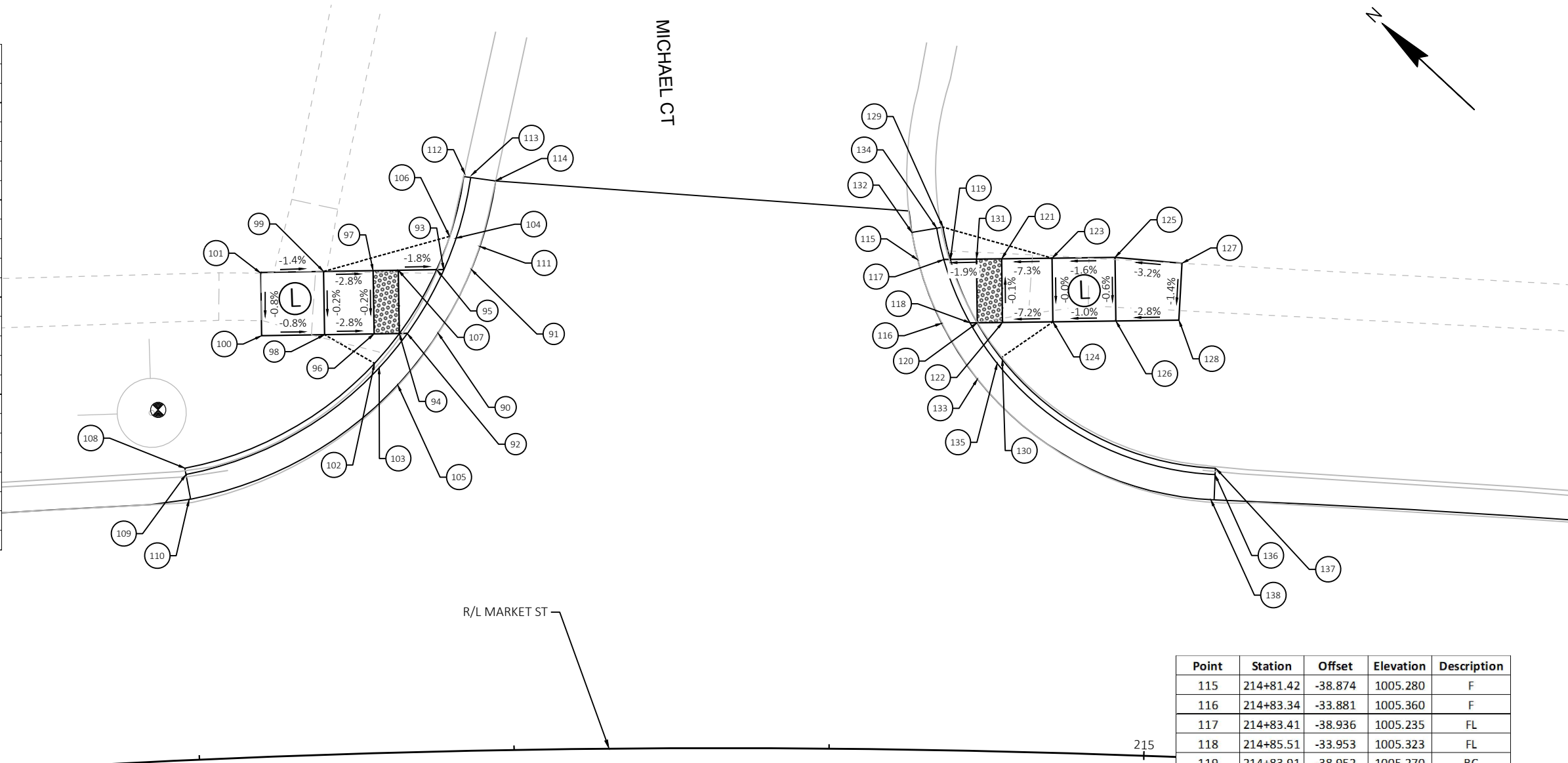
PROJECT NO: 3840-06-71	HWY: MARKET STREET	COUNTY: WALWORTH	PLAN DETAILS	SHEET	E
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NOTES:
 1) SEE CURB RAMP DETAIL SHEETS FOR
 ADDITIONAL INFORMATION.



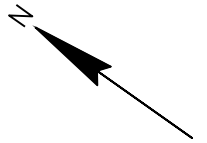
PROJECT NO: 3840-06-71	HWY: MARKET STREET	COUNTY: WALWORTH	PLAN DETAILS	SHEET	E
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Point	Station	Offset	Elevation	Description
90	214+44.72	-33.212	1005.244	F
91	214+47.36	-38.215	1005.188	F
92	214+42.37	-33.240	1005.177	FL
93	214+45.26	-38.237	1005.121	FL
94	214+41.76	-33.246	1005.190	BC/DWF
95	214+44.73	-38.240	1005.134	BC
96	214+39.83	-33.269	1005.231	DWF
97	214+39.89	-38.291	1005.240	DWF
98	214+36.04	-33.327	1005.341	LP
99	214+36.12	-38.348	1005.350	LP
100	214+31.21	-33.424	1005.380	LP (MATCH)
101	214+31.32	-38.381	1005.419	LP (MATCH)
102	214+39.80	-30.925	1005.692	BC
103	214+40.14	-30.575	1005.192	FL
104	214+46.22	-40.684	1005.100	FL
105	214+41.53	-29.175	1005.267	F
106	214+45.78	-40.849	1005.594	BC
107	214+41.81	-38.268	1005.190	DWF
108	214+24.88	-23.161	1005.760	BC (MATCH)
109	214+24.95	-22.664	1005.290	FL (MATCH)
110	214+25.10	-20.665	1005.342	F (MATCH)
111	214+48.03	-40.018	1005.169	F
112	214+47.05	-45.763	1005.149	BC (MATCH)
113	214+47.51	-45.645	1005.045	FL (MATCH)
114	214+49.37	-45.186	1005.110	F (MATCH)

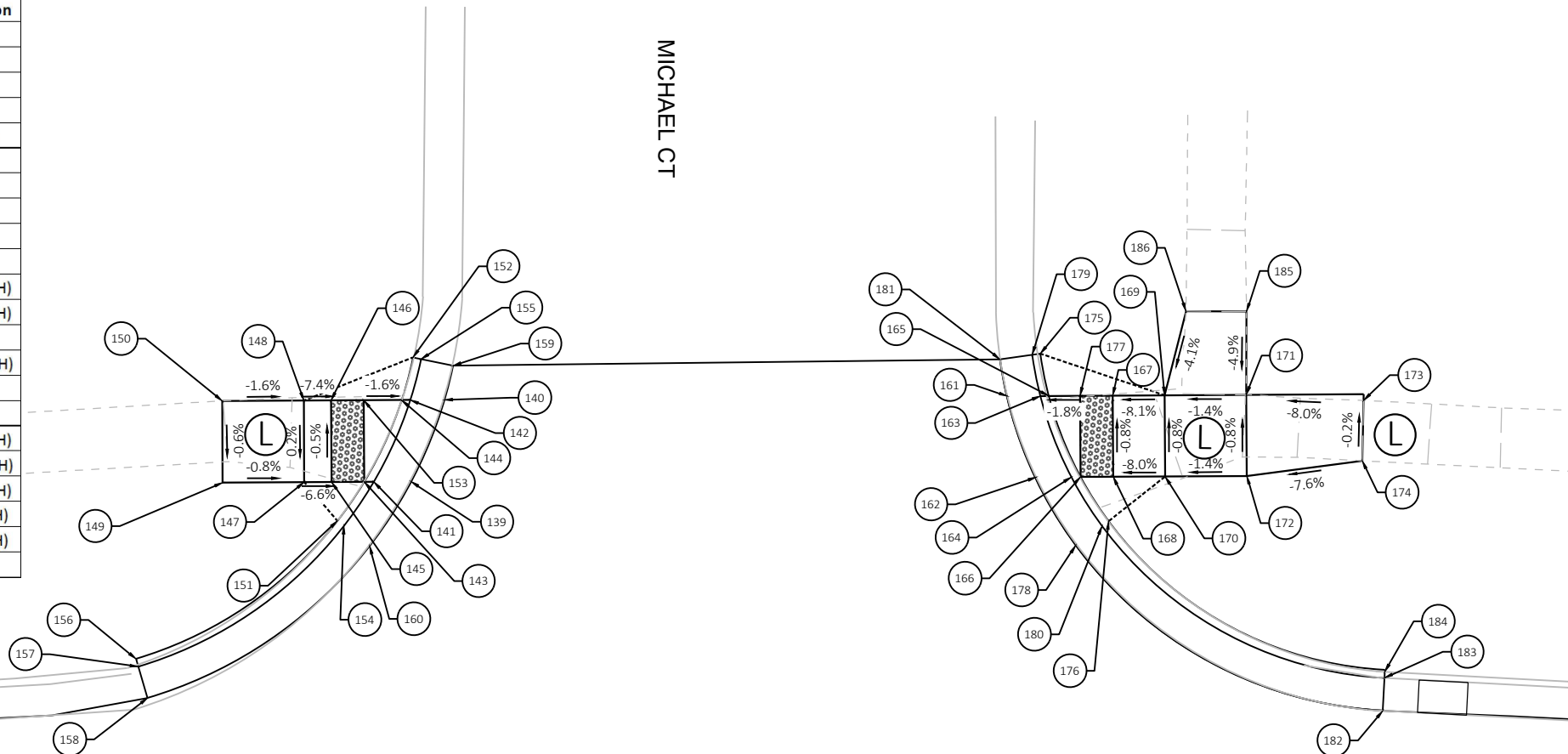


LEGEND	
(L)	LEVEL LANDING
BC	BACK OF CURB
DWF	DETECTABLE WARNING FIELD
F	FLANGE
FL	FLOWLINE
GB	GRADE BREAK
LP	LANDING PAD
R	RAMP
S	SIDEWALK

Point	Station	Offset	Elevation	Description
115	214+81.42	-38.874	1005.280	F
116	214+83.34	-33.881	1005.360	F
117	214+83.41	-38.936	1005.235	FL
118	214+85.51	-33.953	1005.323	FL
119	214+83.91	-38.952	1005.270	BC
120	214+86.06	-33.979	1005.340	BC/DWF
121	214+87.82	-39.089	1005.487	DWF
122	214+87.99	-34.042	1005.493	DWF
123	214+91.66	-39.240	1005.780	LP
124	214+91.85	-34.195	1005.780	LP
125	214+96.45	-39.453	1005.860	LP
126	214+96.67	-34.408	1005.830	LP
127	215+01.56	-39.165	1006.031	S (MATCH)
128	215+01.48	-34.647	1005.969	S (MATCH)
129	214+83.30	-41.502	1005.994	BC (MATCH)
130	214+88.03	-31.116	1005.890	BC
131	214+85.90	-39.020	1005.310	DWF
132	214+80.95	-41.038	1005.249	F (MATCH)
133	214+86.17	-29.514	1005.443	F
134	214+82.83	-41.409	1005.168	FL (MATCH)
135	214+87.66	-30.796	1005.390	FL
136	215+04.73	-22.512	1005.683	FL (MATCH)
137	215+04.79	-22.996	1005.952	BC (MATCH)
138	215+04.48	-20.506	1005.779	F (MATCH)



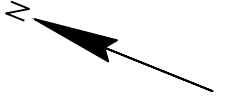
Point	Station	Offset	Elevation	Description
139	215+72.49	-34.032	1005.950	F
140	215+74.56	-38.990	1005.903	F
141	215+70.27	-34.078	1005.907	FL
142	215+72.55	-39.028	1005.855	FL
143	215+69.71	-34.090	1005.960	BC/DWF
144	215+72.04	-39.038	1005.892	BC
145	215+67.78	-34.136	1006.100	DWF
146	215+67.90	-39.131	1006.077	DWF
147	215+66.17	-34.178	1006.210	LP
148	215+66.29	-39.172	1006.200	LP
149	215+61.35	-34.319	1006.250	LP (MATCH)
150	215+61.50	-39.312	1006.281	LP (MATCH)
151	215+68.06	-31.747	1006.433	BC
152	215+72.78	-41.633	1006.332	BC (MATCH)
153	215+69.81	-39.085	1005.930	DWF
154	215+68.43	-31.429	1005.933	FL
155	215+73.24	-41.519	1005.795	FL (MATCH)
156	215+55.79	-23.728	1006.087	BC (MATCH)
157	215+55.91	-23.252	1005.974	FL (MATCH)
158	215+56.39	-21.314	1006.042	F (MATCH)
159	215+75.11	-41.064	1005.885	F (MATCH)
160	215+69.93	-30.158	1005.986	F



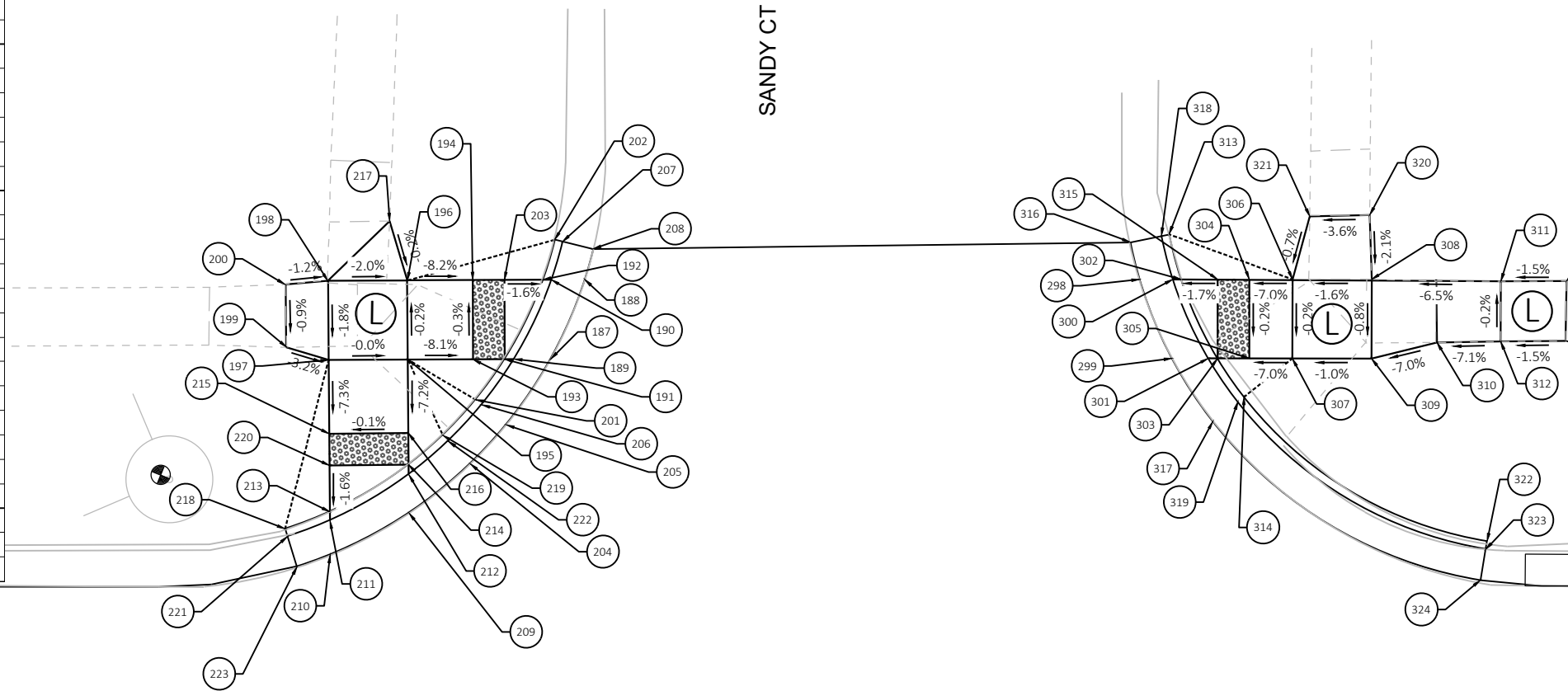
Point	Station	Offset	Elevation	Description
161	216+07.68	-39.068	1006.098	F
162	216+09.45	-34.127	1006.236	F
163	216+09.62	-39.110	1005.999	FL
164	216+11.53	-34.175	1006.078	FL
165	216+10.11	-39.121	1006.035	BC
166	216+12.07	-34.191	1006.110	BC/DWF
167	216+13.87	-39.215	1006.232	DWF
168	216+14.00	-34.242	1006.270	DWF
169	216+16.93	-39.302	1006.490	LP
170	216+17.07	-34.327	1006.530	LP
171	216+21.73	-39.462	1006.560	LP
172	216+21.89	-34.495	1006.600	LP
173	216+28.64	-39.351	1007.135	S (MATCH)
174	216+28.68	-35.650	1007.144	S (MATCH)
175	216+09.58	-41.698	1006.462	BC (MATCH)
176	216+13.77	-31.514	1006.630	BC
177	216+11.95	-39.165	1006.070	DWF
178	216+11.86	-29.985	1006.183	F
179	216+09.11	-41.622	1005.984	FL (MATCH)
180	216+13.40	-31.203	1006.131	FL
181	216+07.21	-41.319	1006.013	F (MATCH)
182	216+30.45	-20.392	1006.485	F (MATCH)
183	216+30.47	-22.409	1006.386	FL (MATCH)
184	216+30.48	-22.774	1006.789	BC (MATCH)
185	216+21.61	-44.554	1006.808	S (MATCH)
186	216+18.06	-44.490	1006.710	S (MATCH)

LEGEND

- LEVEL LANDING
- BC BACK OF CURB
- DWF DETECTABLE WARNING FIELD
- F FLANGE
- FL FLOWLINE
- GB GRADE BREAK
- LP LANDING PAD
- R RAMP
- S SIDEWALK



Point	Station	Offset	Elevation	Description
187	218+71.91	-34.748	1007.561	F
188	218+74.06	-39.773	1007.478	F
189	218+69.60	-34.749	1007.525	FL
190	218+71.95	-39.759	1007.435	FL
191	218+69.01	-34.746	1007.530	BC/DWF
192	218+71.42	-39.757	1007.480	BC
193	218+67.01	-34.738	1007.705	DWF
194	218+66.99	-39.740	1007.690	DWF
195	218+62.88	-34.736	1008.040	LP
196	218+62.86	-39.719	1008.030	LP
197	218+57.89	-34.697	1008.040	LP
198	218+57.84	-39.686	1008.130	LP
199	218+55.24	-35.491	1008.128	S (MATCH)
200	218+55.13	-39.462	1008.163	S (MATCH)
201	218+67.18	-32.178	1008.066	BC
202	218+72.16	-42.284	1007.893	BC (MATCH)
203	218+68.99	-39.746	1007.520	DWF
204	218+66.83	-28.125	1007.613	F
205	218+69.11	-30.594	1007.619	F/GB
206	218+67.56	-31.865	1007.566	FL
207	218+72.65	-42.168	1007.349	FL (MATCH)
208	218+74.60	-41.711	1007.447	F (MATCH)
209	218+62.97	-25.022	1007.603	F
210	218+57.98	-22.393	1007.594	F
211	218+57.97	-24.556	1007.518	FL
212	218+62.95	-27.453	1007.533	FL
213	218+57.96	-25.113	1007.530	BC
214	218+62.94	-28.075	1007.580	BC/DWF
215	218+57.92	-30.034	1007.697	DWF
216	218+62.92	-30.075	1007.703	DWF
217	218+61.74	-43.466	1008.038	S (MATCH)
218	218+55.13	-24.032	1008.020	BC (MATCH)
219	218+65.10	-29.932	1008.038	BC
220	218+57.94	-28.034	1007.580	DWF
221	218+55.28	-23.549	1007.508	FL (MATCH)
222	218+65.44	-29.564	1007.538	FL
223	218+55.88	-21.640	1007.590	F (MATCH)



Point	Station	Offset	Elevation	Description
298	219+09.09	-39.788	1007.521	F
299	219+11.14	-34.794	1007.544	F
300	219+11.18	-39.783	1007.480	FL
301	219+13.44	-34.792	1007.500	FL
302	219+11.71	-39.784	1007.500	BC
303	219+14.02	-34.793	1007.530	BC/DWF
304	219+16.02	-39.787	1007.680	DWF
305	219+16.01	-34.794	1007.670	DWF
306	219+18.75	-39.783	1007.870	LP
307	219+18.74	-34.789	1007.860	LP
308	219+23.74	-39.777	1007.950	LP
309	219+23.75	-34.780	1007.910	LP
310	219+27.88	-35.798	1008.170	S
311	219+31.91	-39.665	1008.401	S
312	219+31.89	-35.857	1008.413	S
313	219+10.96	-42.636	1007.520	BC (MATCH)
314	219+15.69	-32.368	1007.990	BC
315	219+14.02	-39.787	1007.540	DWF
316	219+08.50	-42.132	1007.509	F (MATCH)
317	219+13.73	-30.823	1007.560	F
318	219+10.47	-42.534	1007.470	FL (MATCH)
319	219+15.30	-32.064	1007.520	FL
320	219+23.62	-43.841	1008.036	S (MATCH)
321	219+19.84	-43.759	1007.899	S (MATCH)
322	219+31.03	-23.251	1008.120	BC (MATCH)
323	219+30.94	-22.728	1007.652	FL (MATCH)
324	219+30.61	-20.790	1007.693	F (MATCH)
767	219+35.99	-35.892	1008.56	S (MATCH)
768	219+36.05	-39.674	1008.55	S (MATCH)

LEGEND

- LEVEL LANDING
- BC** BACK OF CURB
- DWF** DETECTABLE WARNING FIELD
- F** FLANGE
- FL** FLOWLINE
- GB** GRADE BREAK
- LP** LANDING PAD
- R** RAMP
- S** SIDEWALK

R/L MARKET ST

MARKET ST

219

R/L MARKET ST

219



MARKET ST

Point	Station	Offset	Elevation	Description
224	218+63.97	25.236	1007.633	F
225	218+58.91	22.236	1007.588	F
226	218+64.00	27.821	1007.566	FL
227	218+58.93	24.471	1007.534	FL
228	218+64.01	28.488	1007.604	BC/DWF
229	218+58.94	25.035	1007.560	BC
230	218+64.03	30.488	1007.717	DWF
231	218+59.00	30.545	1007.818	DWF
232	218+64.07	33.741	1007.900	LP
233	218+59.04	33.783	1008.000	LP
234	218+64.09	38.789	1007.920	LP
235	218+59.09	38.796	1008.010	LP
236	218+66.02	30.471	1008.058	BC
237	218+56.32	23.874	1008.020	BC
238	218+58.99	28.545	1007.620	DWF
239	218+67.15	31.014	1007.573	FL
240	218+57.20	21.533	1007.593	F
241	218+66.39	30.133	1007.570	FL
242	218+66.14	27.044	1007.647	F/GB
243	218+56.50	23.406	1007.520	FL
244	218+54.29	38.710	1008.238	S (MATCH)
245	218+54.36	34.750	1008.230	S (MATCH)
246	218+73.65	38.790	1007.570	F
247	218+71.45	33.747	1007.600	F
248	218+71.54	38.784	1007.520	FL
249	218+69.13	33.760	1007.550	FL
250	218+71.01	38.794	1007.530	BC
251	218+68.54	33.769	1007.595	BC/DWF
252	218+66.54	38.801	1007.748	DWF
253	218+66.54	33.762	1007.735	DWF
254	218+62.93	43.809	1008.106	S (MATCH)
255	218+59.00	43.786	1008.175	S (MATCH)
256	218+68.54	38.798	1007.570	DWF
257	218+74.29	41.087	1007.557	F
258	218+66.77	31.333	1008.052	BC
259	218+72.34	41.534	1007.505	FL
260	218+71.85	41.646	1007.530	BC
261	218+40.25	19.761	1007.507	F (MATCH)
262	218+40.21	21.936	1007.367	FL (MATCH)
263	218+40.22	22.349	1007.831	BC (MATCH)
264	218+72.47	46.761	1007.522	BC (MATCH)
265	218+72.98	46.766	1007.349	FL (MATCH)
266	218+74.98	46.871	1007.514	F (MATCH)



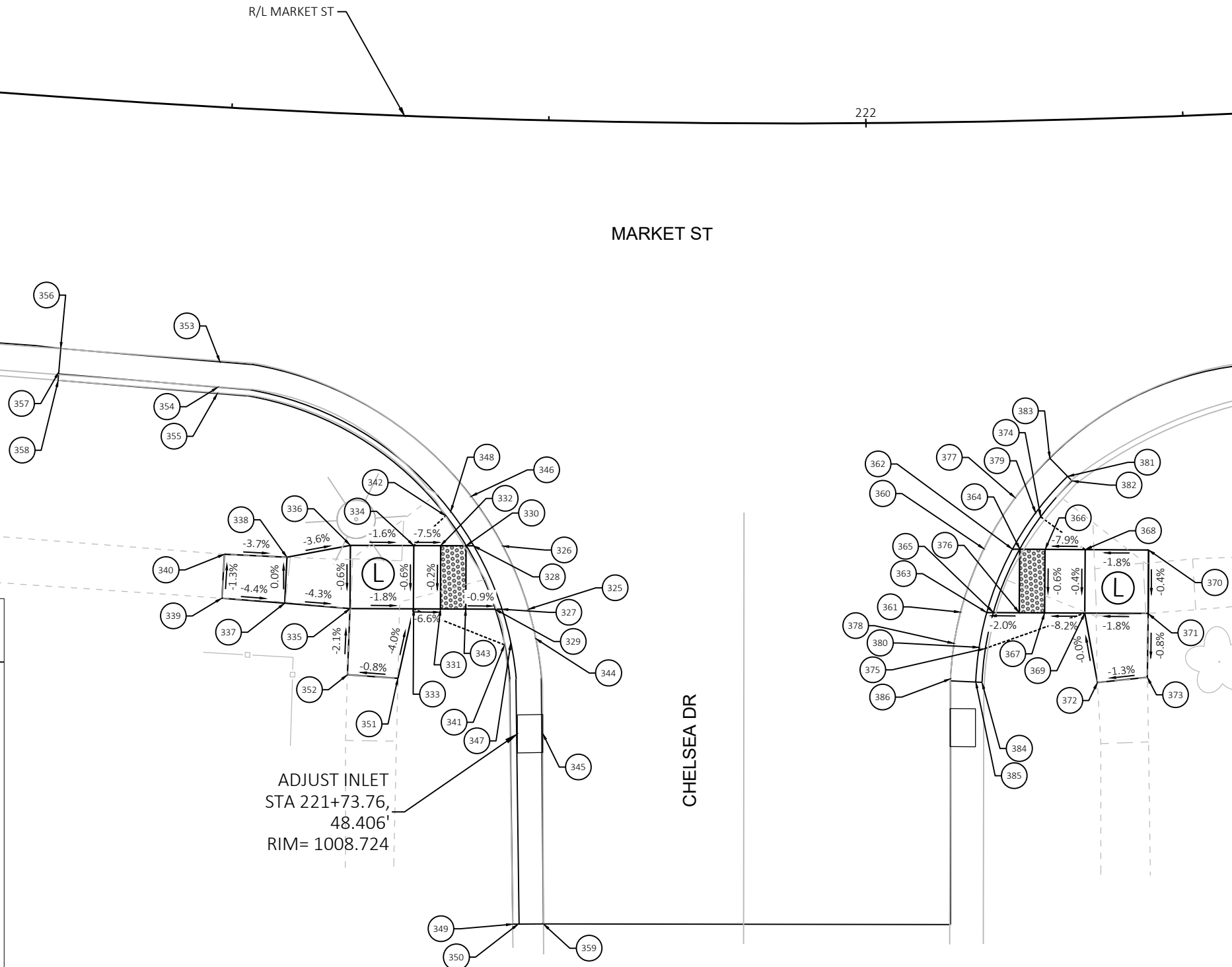
LEGEND

- LEVEL LANDING
- BC BACK OF CURB
- DWF DETECTABLE WARNING FIELD
- F FLANGE
- FL FLOWLINE
- GB GRADE BREAK
- LP LANDING PAD
- R RAMP
- S SIDEWALK

Point	Station	Offset	Elevation	Description
267	219+10.92	33.733	1007.687	F
268	219+08.69	38.722	1007.613	F
269	219+13.23	33.725	1007.630	FL
270	219+10.79	38.723	1007.550	FL
271	219+13.82	33.728	1007.630	BC/DWF
272	219+11.32	38.721	1007.590	BC
273	219+15.82	33.728	1007.800	DWF
274	219+15.83	38.715	1007.820	DWF
275	219+18.25	33.720	1007.970	LP
276	219+18.26	38.708	1007.990	LP
277	219+23.25	33.716	1008.010	LP
278	219+23.26	38.704	1008.050	LP
279	219+28.63	34.806	1008.210	S
280	219+28.58	38.605	1008.280	S
281	219+33.94	34.870	1008.412	S (MATCH)
282	219+33.90	38.508	1008.515	S (MATCH)
283	219+15.72	31.183	1008.180	BC
284	219+10.52	41.269	1007.693	BC (MATCH)
285	219+13.83	38.716	1007.630	DWF
286	219+13.81	29.566	1007.750	F
287	219+08.09	40.653	1007.586	F (MATCH)
288	219+15.33	30.864	1007.680	FL
289	219+10.04	41.150	1007.487	FL (MATCH)
290	219+30.03	22.706	1008.271	BC
291	219+29.93	22.236	1007.732	FL
292	219+29.53	20.252	1007.769	F
293	219+39.40	22.195	1008.367	BC (MATCH)
294	219+39.41	21.696	1007.828	FL (MATCH)
295	219+39.42	19.695	1007.868	F (MATCH)
296	219+19.33	43.762	1008.177	S (MATCH)
297	219+23.12	43.682	1008.231	S (MATCH)

Point	Station	Offset	Elevation	Description
325	221+74.34	38.568	1008.840	F
326	221+72.27	33.627	1008.860	F
327	221+72.34	38.617	1008.770	FL
328	221+70.07	33.683	1008.780	FL
329	221+71.83	38.625	1008.770	BC
330	221+69.51	33.704	1008.781	BC/DWF
331	221+67.73	38.737	1008.930	DWF
332	221+67.59	33.753	1008.940	DWF
333	221+65.71	38.800	1009.070	LP
334	221+65.55	33.816	1009.100	LP
335	221+60.94	38.969	1009.150	LP
336	221+60.76	33.990	1009.180	LP
337	221+56.01	38.786	1009.375	S
338	221+56.01	35.161	1009.364	S
339	221+51.27	38.635	1009.593	S (MATCH)
340	221+51.29	35.170	1009.548	S (MATCH)
341	221+72.62	41.417	1009.260	BC
342	221+67.89	31.382	1009.290	BC
343	221+69.63	38.684	1008.790	DWF
344	221+74.93	40.853	1008.830	F
345	221+75.64	48.327	1008.811	F/GB
346	221+69.76	29.801	1008.870	F
347	221+73.08	41.298	1008.760	FL
348	221+68.27	31.063	1008.790	FL
349	221+73.77	63.410	1009.206	BC (MATCH)
350	221+74.17	63.397	1009.072	FL (MATCH)
351	221+64.73	44.318	1009.294	S (MATCH)
352	221+60.94	44.195	1009.262	S (MATCH)
353	221+50.16	20.041	1008.940	F
354	221+50.12	22.041	1008.821	FL
355	221+50.09	22.541	1009.315	BC
356	221+37.86	19.790	1008.978	F (MATCH)
357	221+37.85	21.621	1008.839	FL (MATCH)
358	221+37.84	22.297	1009.330	BC (MATCH)
359	221+76.02	63.342	1009.159	F (MATCH)

Point	Station	Offset	Elevation	Description
360	222+08.71	33.687	1008.982	F
361	222+06.85	38.647	1008.947	F
362	222+10.85	33.743	1008.940	FL
363	222+08.82	38.701	1008.900	FL
364	222+11.41	33.761	1008.990	BC/DWF
365	222+09.32	38.712	1008.920	BC
366	222+13.32	33.821	1009.150	DWF
367	222+13.17	38.828	1009.120	DWF
368	222+16.35	33.920	1009.400	LP
369	222+16.19	38.931	1009.380	LP
370	222+21.15	34.107	1009.490	LP (MATCH)
371	222+20.96	39.116	1009.470	LP (MATCH)
372	222+16.99	44.434	1009.381	S (MATCH)
373	222+20.73	44.132	1009.431	S (MATCH)
374	222+13.05	31.244	1009.470	BC
375	222+08.67	41.535	1009.380	BC
376	222+11.27	38.768	1008.950	DWF
377	222+11.14	29.709	1009.030	F
378	222+06.33	41.101	1008.930	F
379	222+12.67	30.938	1008.970	FL
380	222+08.21	41.440	1008.880	FL
381	222+15.17	28.120	1008.989	FL (MATCH)
382	222+15.49	28.488	1009.539	BC (MATCH)
383	222+13.84	26.660	1009.099	F (MATCH)
384	222+08.38	44.210	1008.982	BC (MATCH)
385	222+07.91	44.142	1008.825	FL (MATCH)
386	222+06.02	43.875	1008.914	F (MATCH)



LEGEND

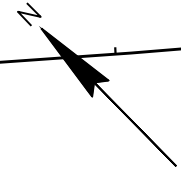
- (L)** LEVEL LANDING
- BC** BACK OF CURB
- DWF** DETECTABLE WARNING FIELD
- F** FLANGE
- FL** FLOWLINE
- GB** GRADE BREAK
- LP** LANDING PAD
- R** RAMP
- S** SIDEWALK

MARKET ST

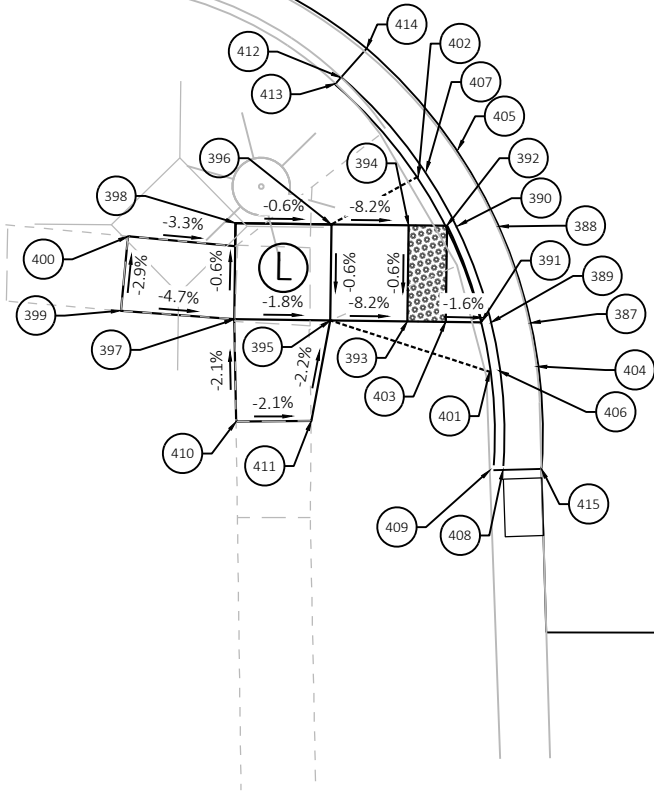
R/L MARKET ST

224

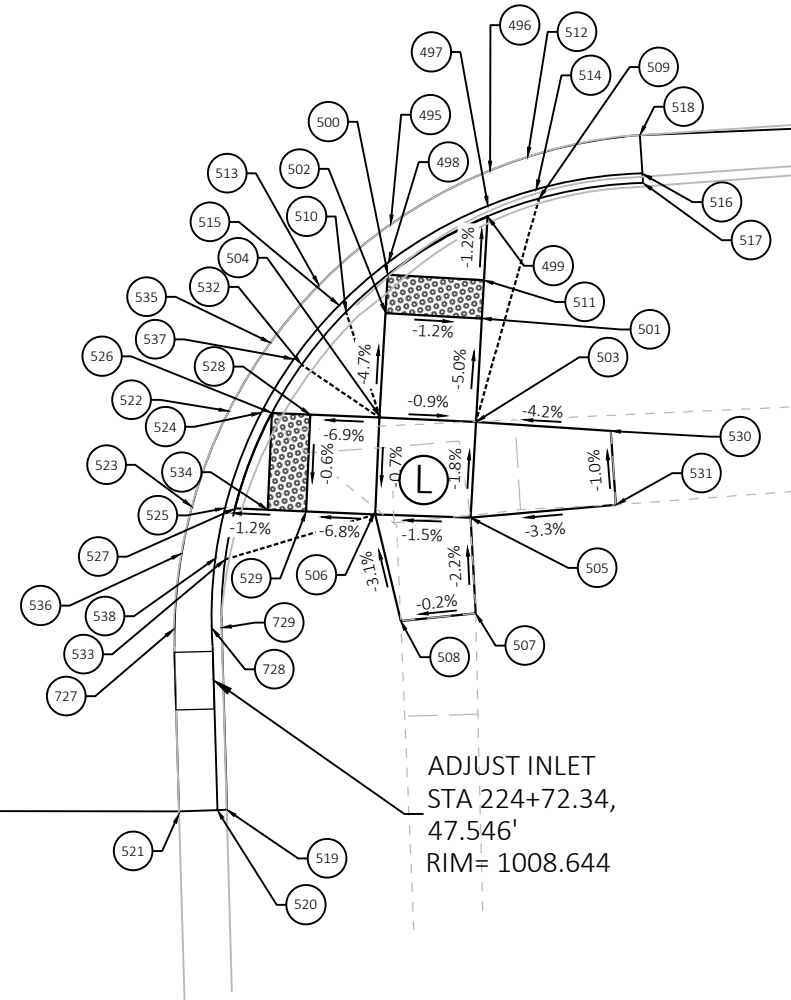
225



Point	Station	Offset	Elevation	Description
387	224+39.52	38.621	1008.758	F
388	224+37.77	33.655	1008.812	F
389	224+37.56	38.664	1008.710	FL
390	224+35.72	33.704	1008.770	FL
391	224+37.06	38.670	1008.750	BC
392	224+35.18	33.718	1008.810	BC/DWF
393	224+33.40	38.771	1008.950	DWF
394	224+33.27	33.774	1008.980	DWF
395	224+29.57	38.896	1009.280	LP
396	224+29.41	33.892	1009.310	LP
397	224+24.80	39.070	1009.370	LP
398	224+24.63	34.067	1009.340	LP
399	224+19.16	38.942	1009.646	S (MATCH)
400	224+19.31	35.058	1009.533	S (MATCH)
401	224+37.56	41.225	1009.180	BC
402	224+33.65	31.208	1009.300	BC
403	224+35.30	38.716	1008.780	DWF
404	224+39.91	40.873	1008.740	F
405	224+35.60	29.732	1008.860	F
406	224+38.03	41.157	1008.680	FL
407	224+34.04	30.914	1008.800	FL
408	224+38.40	46.316	1008.623	FL (MATCH)
409	224+37.75	46.362	1008.807	BC (MATCH)
410	224+25.15	44.387	1009.480	S (MATCH)
411	224+28.86	44.160	1009.398	S (MATCH)
412	224+29.56	26.234	1008.890	FL (MATCH)
413	224+29.30	26.568	1009.367	BC (MATCH)
414	224+30.79	24.659	1008.950	F (MATCH)
415	224+40.29	46.192	1008.743	F (MATCH)



PATRICIA ST



ADJUST INLET
 STA 224+72.34,
 47.546'
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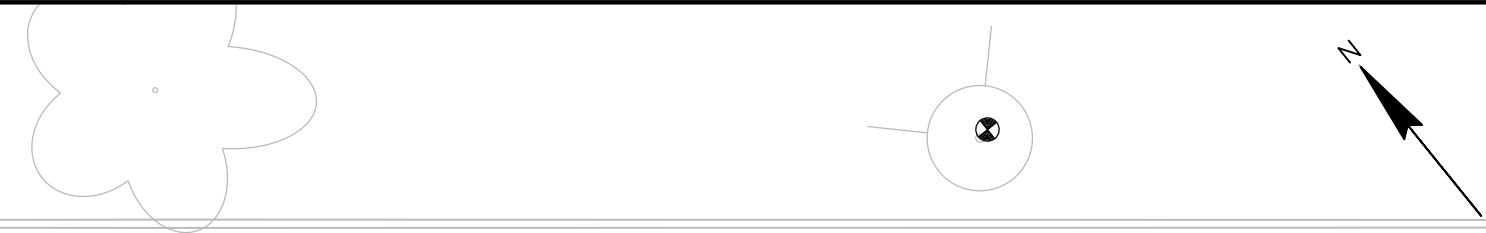
Point	Station	Offset	Elevation	Description
495	224+81.49	23.958	1008.847	F
496	224+86.59	21.270	1008.813	F
497	224+86.44	23.100	1008.740	FL
498	224+81.35	25.905	1008.780	FL
499	224+86.40	23.658	1008.750	BC
500	224+81.30	26.569	1008.820	BC/DWF
501	224+85.97	28.949	1008.860	DWF
502	224+81.16	28.559	1008.920	DWF
503	224+85.56	34.332	1009.130	LP
504	224+80.77	33.937	1009.180	LP
505	224+85.17	39.312	1009.220	LP
506	224+80.41	38.920	1009.140	LP
507	224+85.31	44.295	1009.330	S (MATCH)
508	224+81.60	44.602	1009.321	S (MATCH)
509	224+89.02	22.809	1009.220	BC
510	224+79.16	28.462	1009.300	BC
511	224+86.13	26.959	1008.790	DWF
512	224+88.51	20.609	1008.800	F
513	224+77.82	27.050	1008.876	F
514	224+88.91	22.322	1008.720	FL
515	224+78.82	28.106	1008.800	FL
516	224+94.32	21.667	1008.673	FL (MATCH)
517	224+94.33	22.167	1009.114	BC (MATCH)
518	224+94.26	19.668	1008.763	F (MATCH)
519	224+72.94	54.307	1008.828	BC (MATCH)
520	224+72.47	54.317	1008.707	FL (MATCH)
521	224+70.60	54.489	1008.798	F (MATCH)
522	224+73.21	33.547	1008.710	F
523	224+71.34	38.504	1008.710	F
524	224+74.84	33.637	1008.750	FL
525	224+72.99	38.588	1008.710	FL
526	224+75.37	33.664	1008.790	BC/DWF
527	224+73.48	38.620	1008.740	BC
528	224+77.28	33.778	1008.930	DWF
529	224+77.01	38.821	1008.900	DWF
530	224+92.27	35.010	1009.432	S (MATCH)
531	224+92.40	38.861	1009.470	S (MATCH)
532	224+76.89	31.171	1009.280	BC
533	224+72.97	41.268	1009.190	BC
534	224+75.10	38.708	1008.760	DWF
535	224+75.34	30.004	1008.852	F
536	224+70.78	40.956	1008.763	F
537	224+76.51	30.878	1008.780	FL
538	224+72.49	41.194	1008.690	FL
727	224+70.39	44.880	1008.737	F
728	224+72.29	44.838	1008.657	FL
729	224+72.76	44.825	1009.157	BC

LEGEND

- (L) LEVEL LANDING
- BC BACK OF CURB
- DWF DETECTABLE WARNING FIELD
- F FLANGE
- FL FLOWLINE
- GB GRADE BREAK
- LP LANDING PAD
- R RAMP
- S SIDEWALK

LEGEND

- (L) LEVEL LANDING
- BC BACK OF CURB
- DWF DETECTABLE WARNING FIELD
- F FLANGE
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- R RAMP
- S SIDEWALK



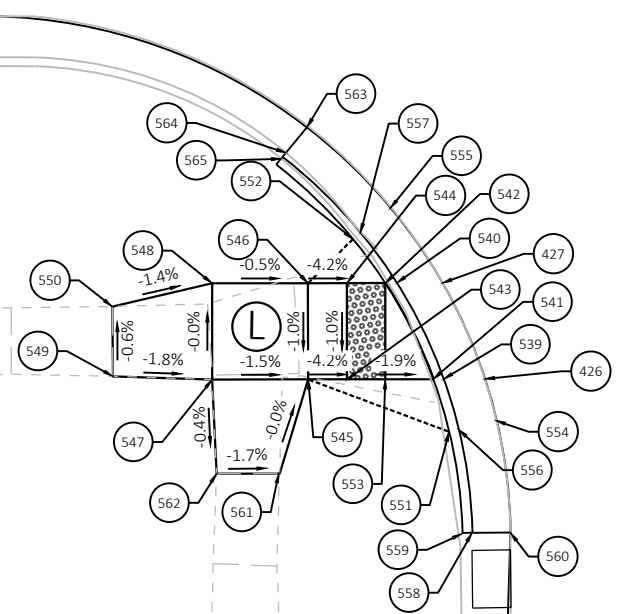
227

228

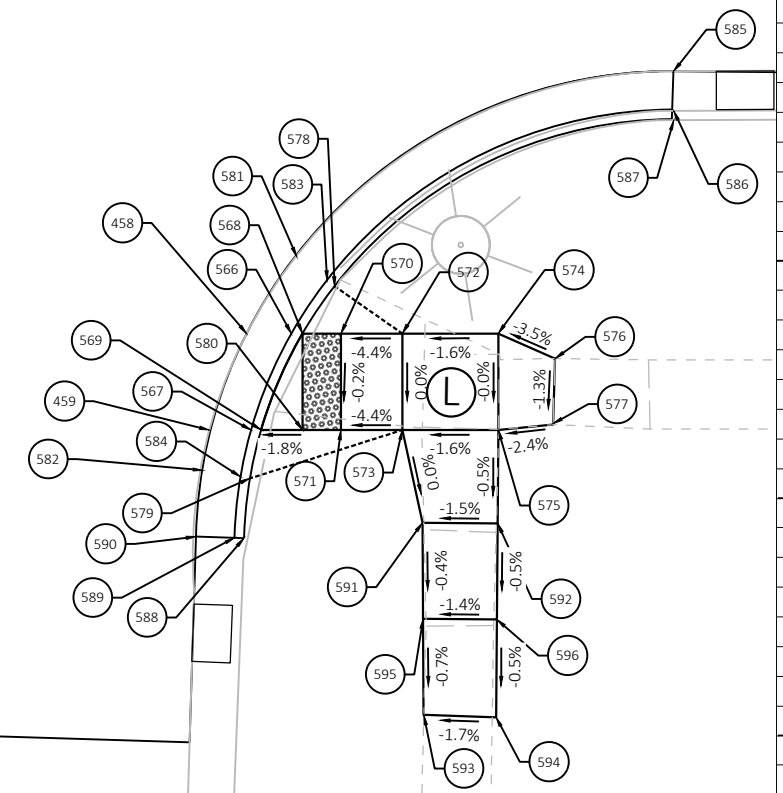
MARKET ST

R/L MARKET ST

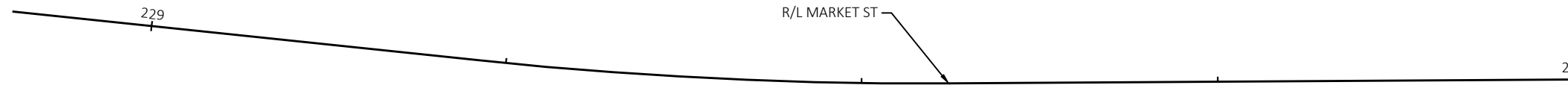
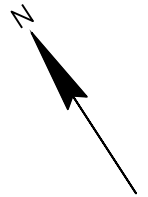
Point	Station	Offset	Elevation	Description
426	227+29.92	38.603	1008.958	F
427	227+27.70	33.599	1008.986	F
539	227+27.76	38.608	1008.880	FL
540	227+25.32	33.600	1008.910	FL
541	227+27.22	38.608	1008.890	BC
542	227+24.72	33.608	1008.950	BC/DWF
543	227+22.73	38.624	1008.980	DWF
544	227+22.73	33.611	1009.030	DWF
545	227+20.71	38.624	1009.064	LP
546	227+20.70	33.617	1009.115	LP
547	227+15.71	38.629	1009.140	LP
548	227+15.70	33.622	1009.140	LP
549	227+10.54	38.457	1009.235	S (MATCH)
550	227+10.51	34.826	1009.214	S (MATCH)
551	227+28.05	41.347	1009.370	BC
552	227+23.04	31.312	1009.430	BC
553	227+24.73	38.615	1008.920	DWF
554	227+30.48	40.766	1008.950	F
555	227+24.97	29.724	1009.010	F/GB
556	227+28.54	41.229	1008.870	FL
557	227+23.43	30.997	1008.930	FL
558	227+29.25	46.611	1008.804	FL (MATCH)
559	227+28.71	46.670	1009.000	BC (MATCH)
560	227+31.25	46.603	1008.913	F (MATCH)
561	227+19.18	43.511	1009.064	S (MATCH)
562	227+15.94	43.535	1009.118	S (MATCH)
563	227+20.64	25.508	1008.991	F (MATCH)
564	227+19.56	26.824	1008.871	FL (MATCH)
565	227+19.30	27.133	1009.347	BC (MATCH)



KELLY LN

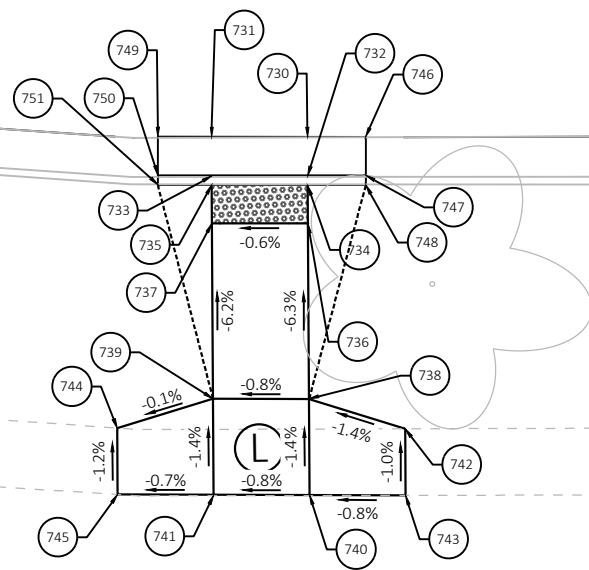


Point	Station	Offset	Elevation	Description
458	227+66.88	33.532	1008.980	F
459	227+64.93	38.552	1008.937	F
566	227+69.21	33.530	1008.900	FL
567	227+67.11	38.549	1008.860	FL
568	227+69.79	33.535	1008.940	BC/DWF
569	227+67.64	38.547	1008.900	BC
570	227+71.79	33.526	1009.080	DWF
571	227+71.79	38.553	1009.080	DWF
572	227+74.99	33.523	1009.220	LP
573	227+74.99	38.550	1009.220	LP
574	227+79.98	33.524	1009.300	LP
575	227+79.99	38.545	1009.300	LP
576	227+82.95	34.825	1009.413	S (MATCH)
577	227+82.84	38.223	1009.368	S (MATCH)
578	227+71.46	31.066	1009.440	BC
579	227+67.05	41.067	1009.330	BC
580	227+69.80	38.548	1008.940	DWF
581	227+69.48	29.528	1009.020	F
582	227+64.59	40.650	1008.910	F
583	227+71.07	30.755	1008.940	FL
584	227+66.56	40.987	1008.830	FL
585	227+89.11	19.871	1009.091	F (MATCH)
586	227+89.12	21.929	1009.014	FL (MATCH)
587	227+89.12	22.420	1009.489	BC (MATCH)
588	227+66.74	44.180	1008.788	BC (MATCH)
589	227+66.24	44.164	1008.706	FL (MATCH)
590	227+64.24	44.103	1008.882	F (MATCH)
591	227+76.03	43.383	1009.220	S
592	227+79.95	43.414	1009.277	S
593	227+76.11	53.400	1009.165	S (MATCH)
594	227+79.87	53.504	1009.230	S (MATCH)
595	227+76.07	48.383	1009.200	S
596	227+79.91	48.414	1009.253	S



MARKET ST

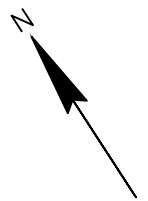
Point	Station	Offset	Elevation	Description
730	229+68.66	19.829	1010.218	F
731	229+63.66	19.790	1010.186	F
732	229+68.66	21.852	1010.142	FL
733	229+63.66	21.836	1010.110	FL
734	229+68.66	22.352	1010.180	BC/DWF
735	229+63.65	22.291	1010.146	BC/DWF
736	229+68.65	24.329	1010.320	DWF
737	229+63.65	24.290	1010.290	DWF
738	229+68.65	33.454	1010.900	LP
739	229+63.65	33.429	1010.860	LP
740	229+68.65	38.476	1010.968	LP
741	229+63.65	38.452	1010.929	LP
742	229+73.65	35.056	1010.972	S (MATCH)
743	229+73.65	38.523	1011.008	S (MATCH)
744	229+58.65	34.924	1010.855	S (MATCH)
745	229+58.65	38.383	1010.895	S (MATCH)
746	229+71.68	19.852	1010.237	F (MATCH)
747	229+71.68	21.852	1010.086	FL (MATCH)
748	229+71.68	22.352	1010.571	BC (MATCH)
749	229+60.87	19.769	1010.168	F (MATCH)
750	229+60.86	21.769	1010.006	FL (MATCH)
751	229+60.86	22.269	1010.502	BC (MATCH)



LEGEND	
(L)	LEVEL LANDING
BC	BACK OF CURB
DWF	DETECTABLE WARNING FIELD
F	FLANGE
FL	FLOWLINE
GB	GRADE BREAK
LP	LANDING PAD
R	RAMP
S	SIDEWALK

LEGEND

- L LEVEL LANDING
- BC BACK OF CURB
- DWF DETECTABLE WARNING FIELD
- F FLANGE
- FL FLOWLINE
- GB GRADE BREAK
- LP LANDING PAD
- R RAMP
- S SIDEWALK

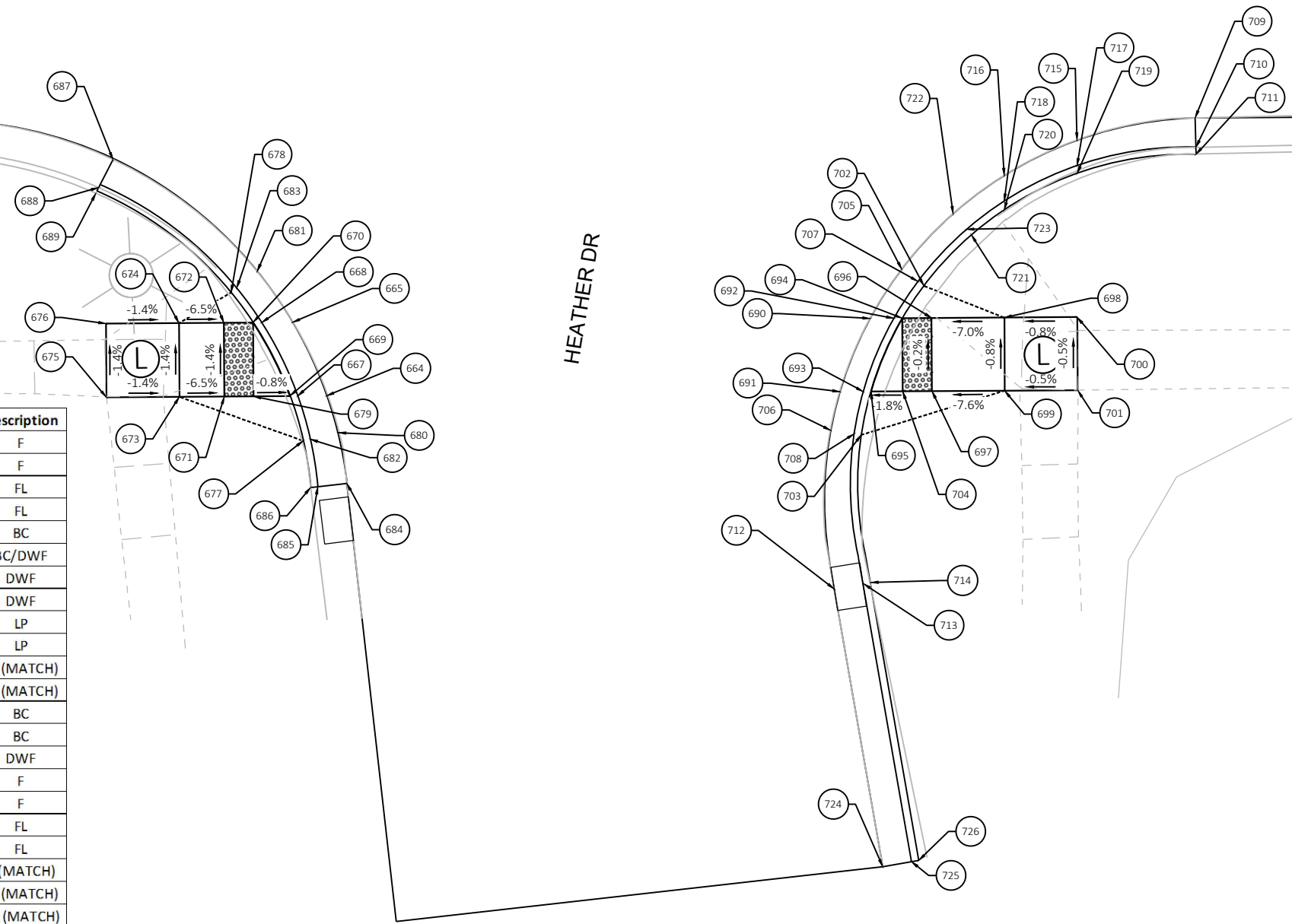


R/L MARKET ST

231

MARKET ST

HEATHER DR

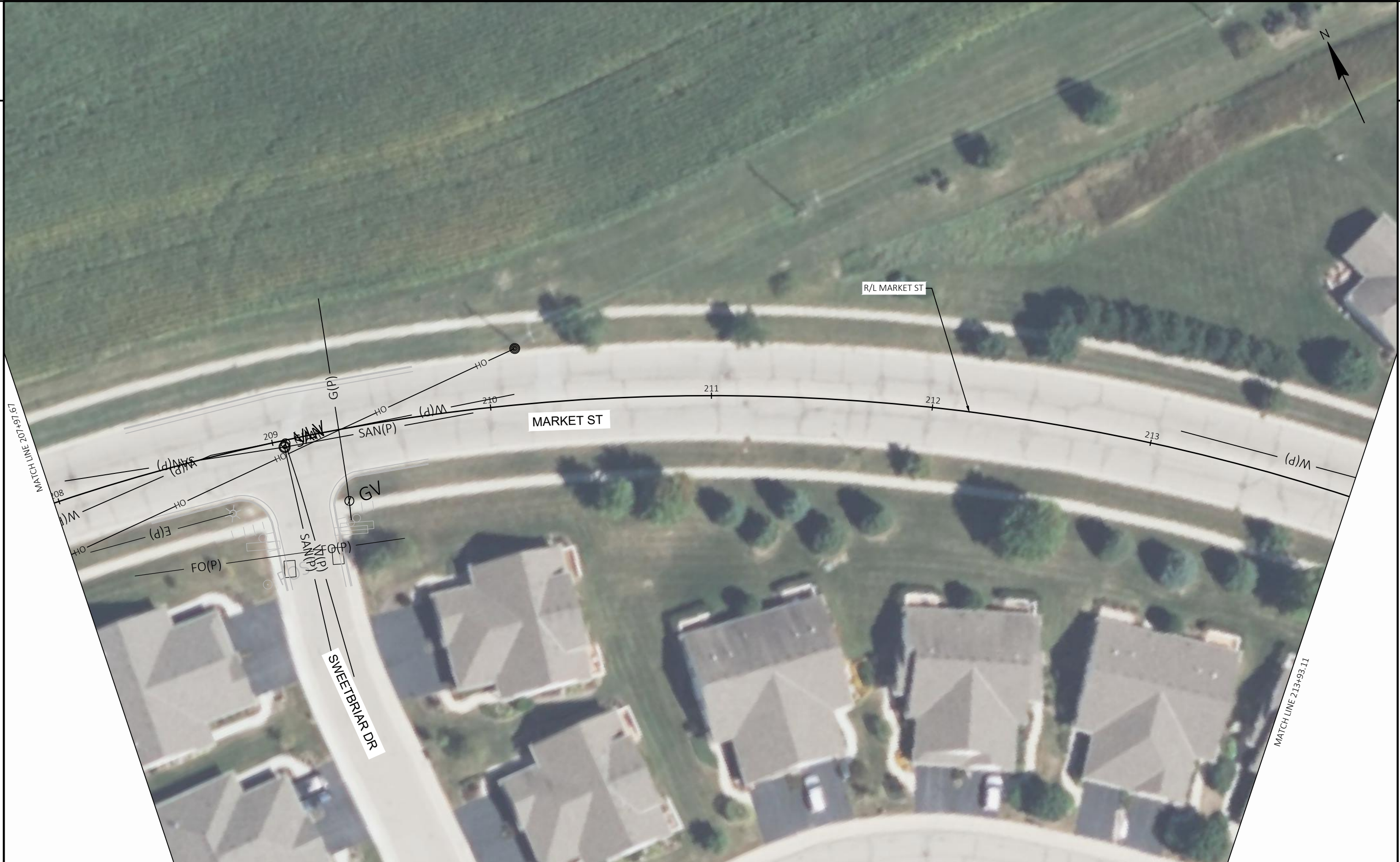


Point	Station	Offset	Elevation	Description
664	230+27.39	38.986	1010.686	F
665	230+25.09	33.941	1010.614	F
667	230+25.39	38.989	1010.600	FL
668	230+22.97	33.941	1010.520	FL
669	230+24.87	38.982	1010.610	BC
670	230+22.37	33.941	1010.560	BC/DWF
671	230+20.38	38.993	1010.770	DWF
672	230+20.38	33.947	1010.700	DWF
673	230+17.31	38.993	1010.970	LP
674	230+17.30	33.947	1010.900	LP
675	230+12.31	39.000	1011.040	LP (MATCH)
676	230+12.31	33.955	1010.970	LP (MATCH)
677	230+25.76	42.058	1011.140	BC
678	230+20.89	31.873	1010.980	BC
679	230+22.38	38.987	1010.630	DWF
680	230+28.19	41.519	1010.720	F
681	230+22.69	30.440	1010.560	F
682	230+26.25	41.947	1010.640	FL
683	230+21.29	31.561	1010.480	FL
684	230+28.74	44.994	1010.796	F (MATCH)
685	230+26.75	45.205	1010.714	FL (MATCH)
686	230+26.25	45.258	1010.887	BC (MATCH)
687	230+12.86	22.726	1010.484	F (MATCH)
688	230+11.87	24.628	1010.397	FL (MATCH)
689	230+11.67	25.026	1010.868	BC (MATCH)

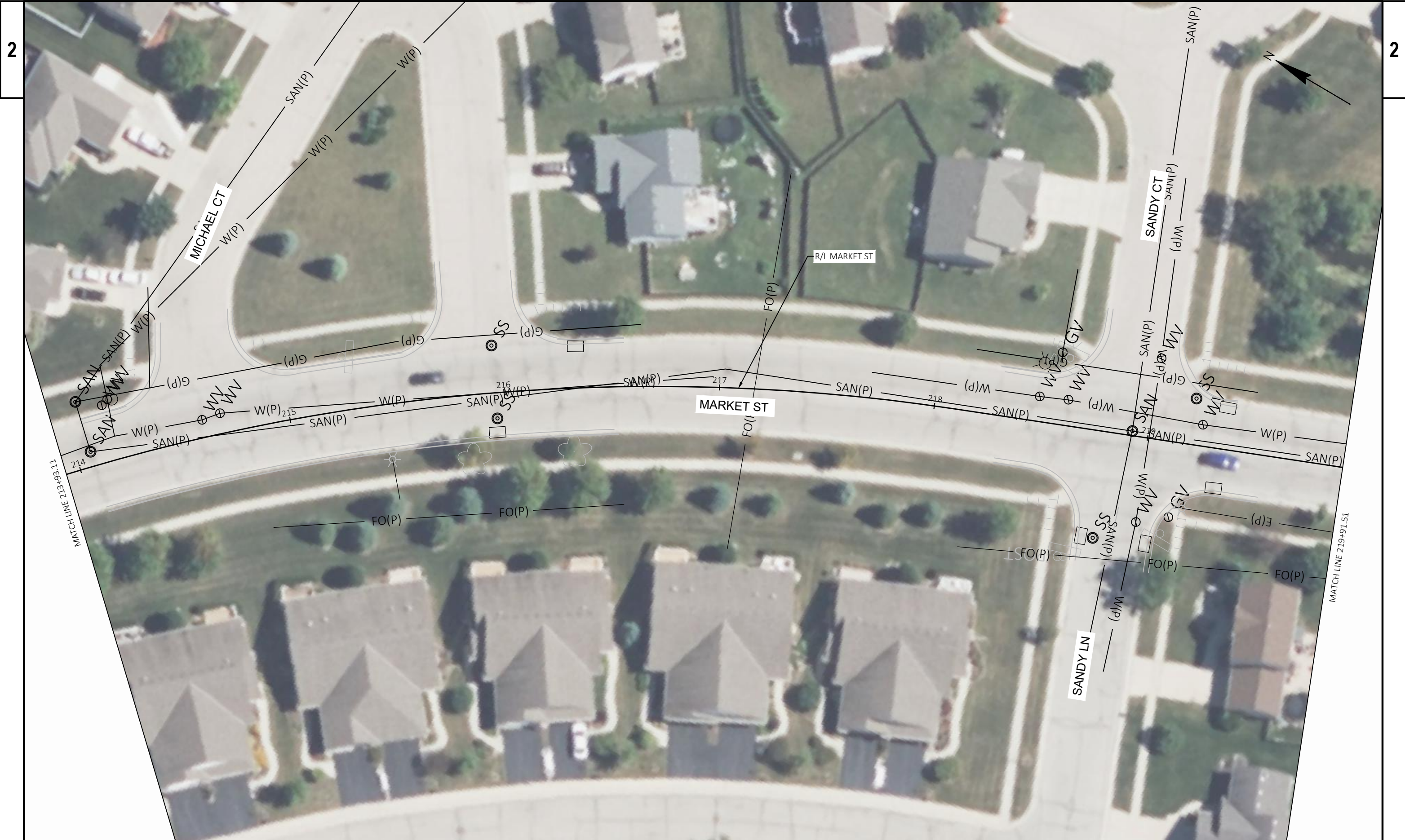
Point	Station	Offset	Elevation	Description
690	230+64.62	33.926	1010.725	F
691	230+62.52	38.928	1010.708	F
692	230+66.25	33.921	1010.650	FL
693	230+64.12	38.929	1010.630	FL
694	230+66.82	33.923	1010.710	BC/DWF
695	230+64.65	38.927	1010.670	BC
696	230+68.83	33.920	1010.860	DWF
697	230+68.82	38.922	1010.870	DWF
698	230+73.80	33.929	1011.210	LP
699	230+73.79	38.931	1011.250	LP (MATCH)
700	230+78.79	33.922	1011.250	LP (MATCH)
701	230+78.80	38.932	1011.274	LP (MATCH)
702	230+68.31	31.709	1011.160	BC
703	230+63.97	41.891	1011.130	BC
704	230+66.82	38.933	1010.710	DWF
705	230+66.80	30.567	1010.737	F
706	230+61.88	41.566	1010.700	F
707	230+67.91	31.405	1010.660	FL
708	230+63.48	41.813	1010.609	FL
709	230+86.95	20.331	1010.808	F (MATCH)
710	230+86.99	22.354	1010.680	FL (MATCH)
711	230+86.99	22.830	1011.175	BC (MATCH)
712	230+62.05	52.432	1010.666	F
713	230+64.07	52.091	1010.540	FL
714	230+64.56	52.008	1011.040	BC
715	230+78.85	21.793	1010.783	F
716	230+73.86	24.193	1010.766	F
717	230+78.85	23.554	1010.710	FL
718	230+73.85	25.950	1010.690	FL
719	230+78.84	24.090	1011.169	BC
720	230+73.85	26.543	1011.165	BC/DWF
721	230+71.59	28.242	1011.163	BC
722	230+70.35	26.787	1010.753	F
723	230+71.26	27.867	1010.680	FL
724	230+65.25	71.469	1011.556	F (MATCH)
725	230+67.22	71.137	1011.427	FL (MATCH)
726	230+67.71	71.055	1011.588	BC (MATCH)



PROJECT NO: 3840-06-71	HWY: MARKET STREET	COUNTY: WALWORTH	EXISTING UTILITY PLAN	SHEET	E
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PROJECT NO: 3840-06-71	HWY: MARKET STREET	COUNTY: WALWORTH	EXISTING UTILITY PLAN	SHEET E
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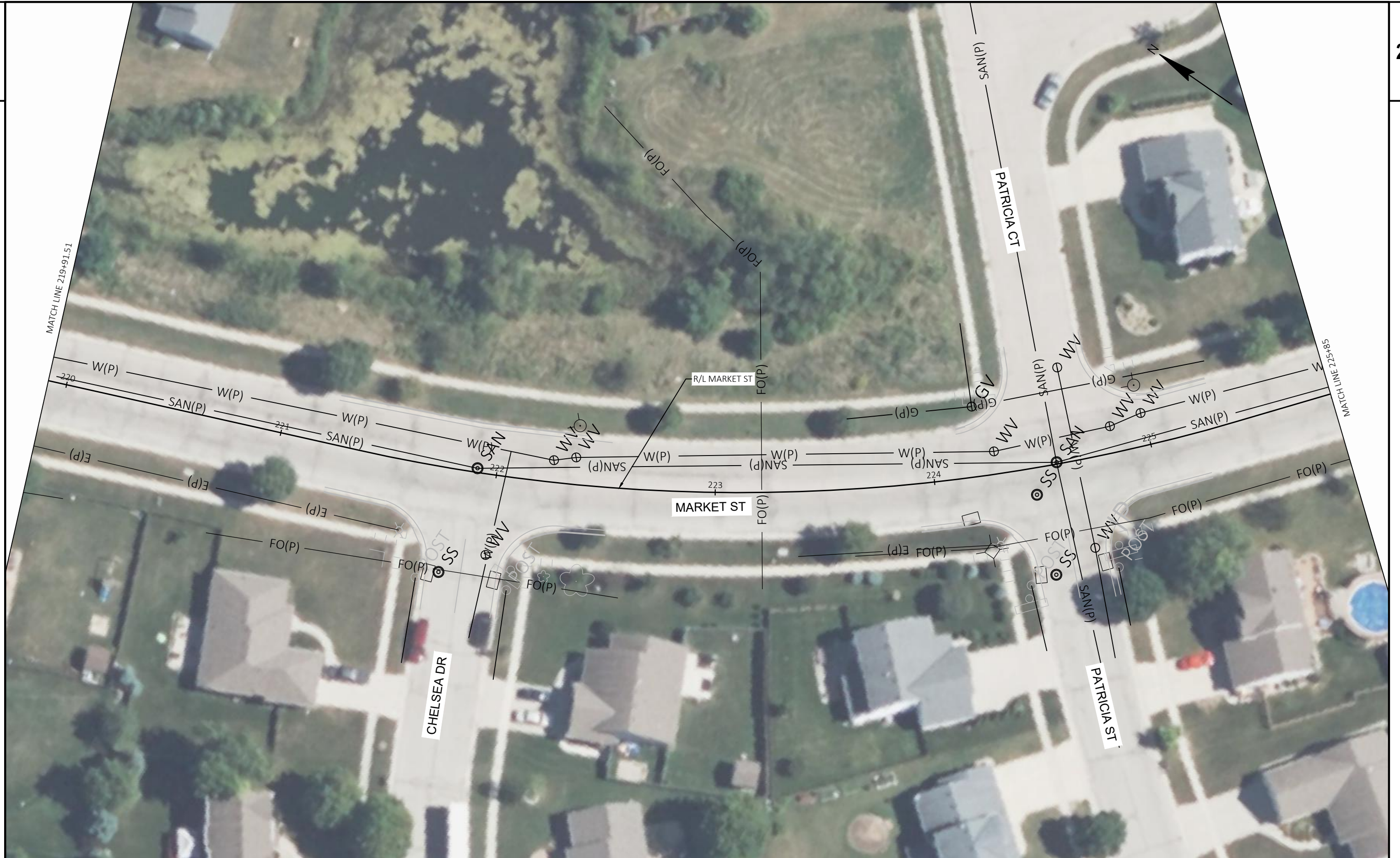


2

2

PROJECT NO: 3840-06-71	HWY: MARKET STREET	COUNTY: WALWORTH	EXISTING UTILITY PLAN	SHEET	E
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FILE NAME : D:\WALWORTH_CO\ELKHORN_CITY\GOV\220543_ELKHORN MARKET ST RESURFACING BIL\34800671\SHEETSPLAN\023001-UP.DWG PLOT DATE : 7/28/2022 12:01 PM PLOT BY : WALTER A. WOLAK II PLOT NAME : PLOT SCALE : 1 IN=40 FT WISDOT/CADD SHEET 42



PROJECT NO: 3840-06-71

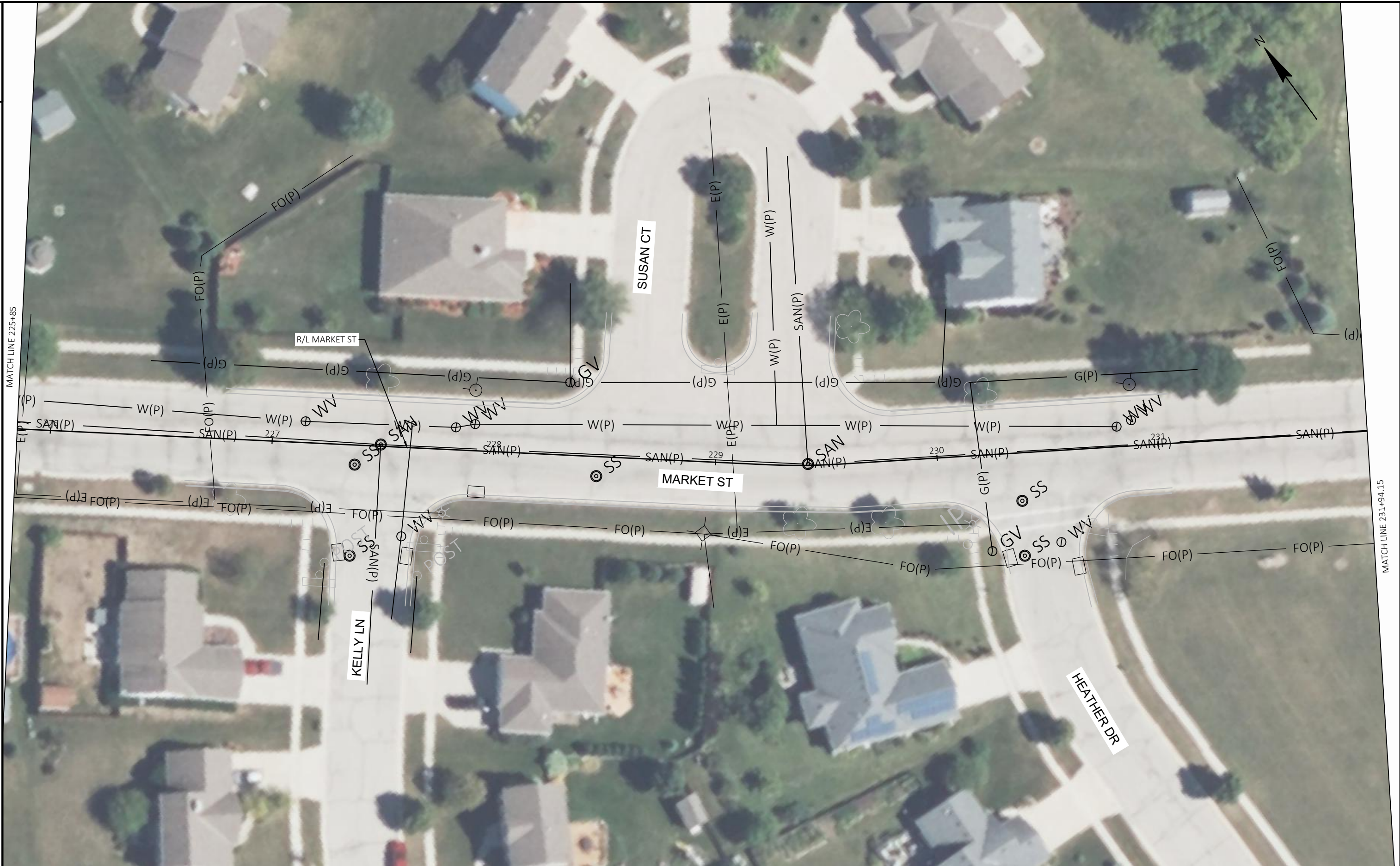
HWY: MARKET STREET

COUNTY: WALWORTH

EXISTING UTILITY PLAN

SHEET

E



PROJECT NO: 3840-06-71	HWY: MARKET STREET	COUNTY: WALWORTH	EXISTING UTILITY PLAN	SHEET	E
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2

2



R/L MARKET ST

MARKET ST

DYLAN DR

MATCH LINE 231+94.15

MATCH LINE 237+92.08

PROJECT NO: 3840-06-71	HWY: MARKET STREET	COUNTY: WALWORTH	EXISTING UTILITY PLAN	SHEET	E
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FILE NAME : D:\WALWORTH_CO\ELKHORN_CITY\GOV\220543_ELKHORN MARKET ST RESURFACING BILL\34800671\SHEETSPLAN\023001-UP.DWG PLOT DATE : 7/28/2022 12:01 PM PLOT BY : WALTER A. WOLAK II PLOT NAME : PLOT SCALE : 1 IN:40 FT WISDOT/CADD SHEET 42



PROJECT NO: 3840-06-71

HWY: MARKET STREET

COUNTY: WALWORTH

EXISTING UTILITY PLAN

SHEET

E



PROJECT NO: 3840-06-71

HWY: MARKET STREET

COUNTY: WALWORTH

EXISTING UTILITY PLAN

SHEET

E

FILE NAME : D:\WALWORTH_CO\ELKHORN_CITY\GOV\220543_ELKHORN MARKET ST RESURFACING BIL\34800671\SHEETSPLAN\023001-UP.DWG
LAYOUT NAME - PD8

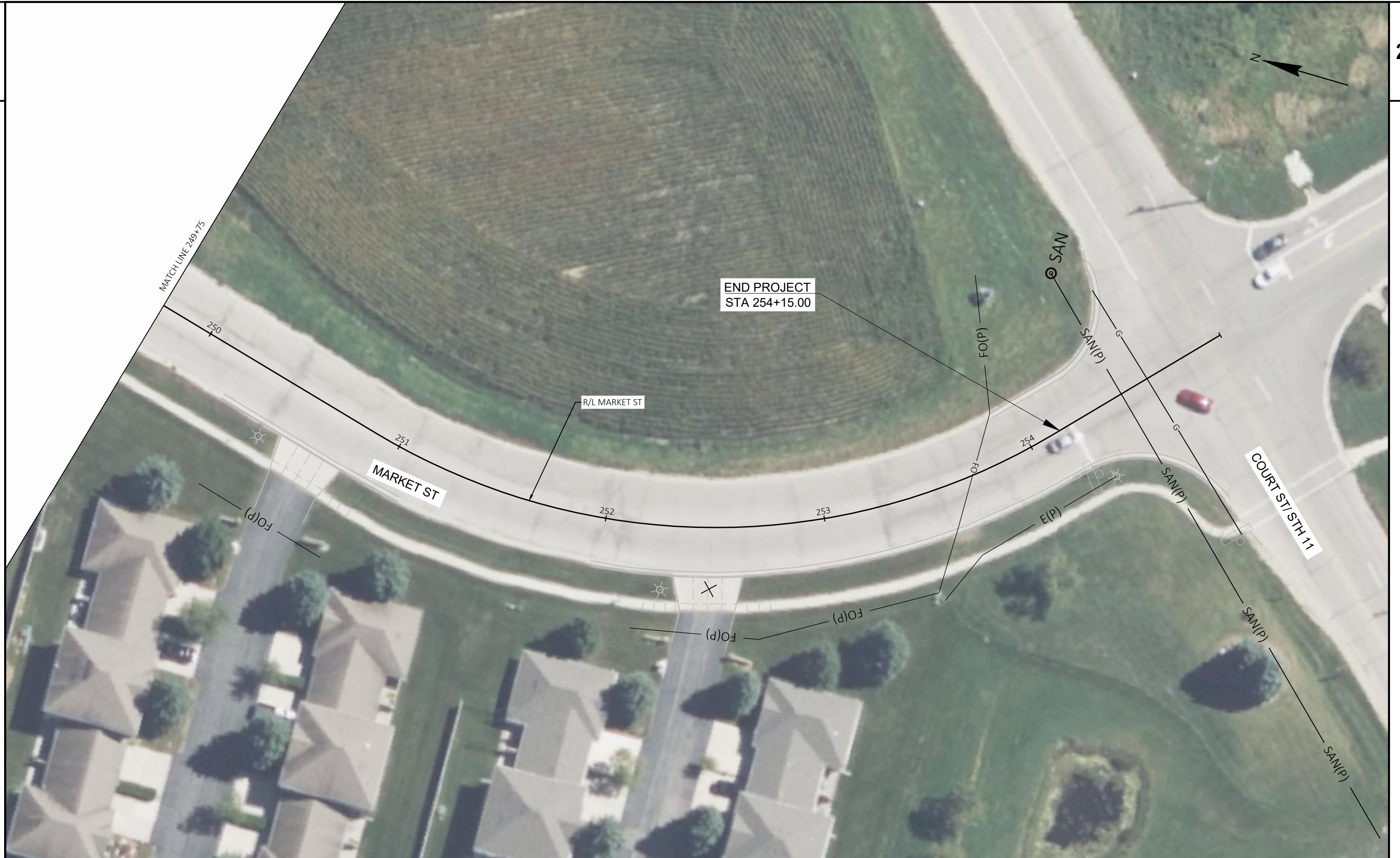
PLOT DATE : 7/28/2022 12:01 PM

PLOT BY : WALTER A. WOLAK II

PLOT NAME :

PLOT SCALE : 1 IN=40 FT

WISDOT/CADD SHEET 42



PROJECT NO: 3840-06-71

HWY: MARKET STREET

COUNTY: WALWORTH








EXISTING UTILITY PLAN

SHEET



E



SIGNING LEGEND

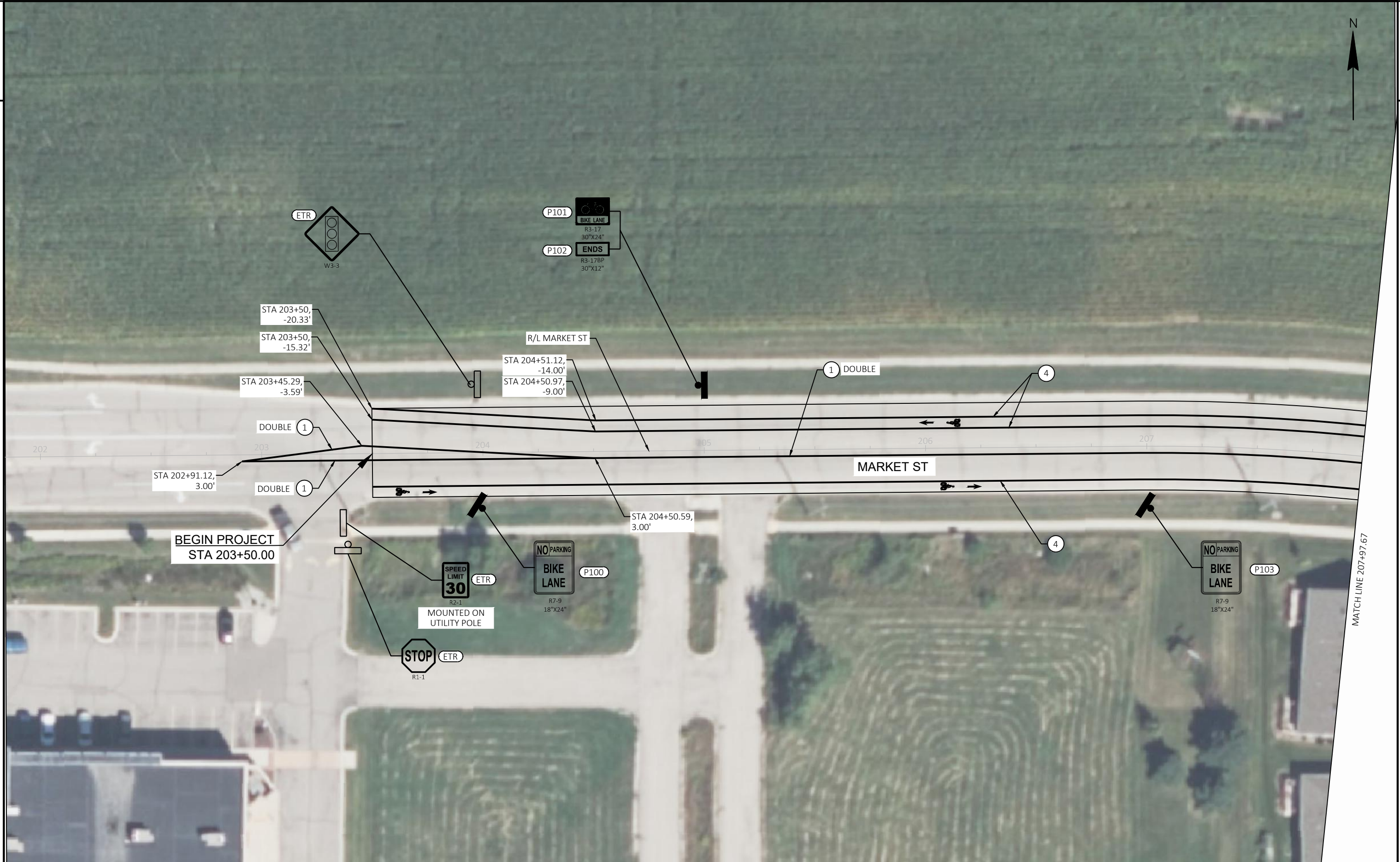
-  EXISTING SIGN MOUNTED ON POST(S)
-  PROPOSED SIGN MOUNTED ON POST(S)
-  PROPOSED SIGN MOUNTED ON EXISTING POST(S)
-  EXISTING SIGN TO BE REMOVED
-  EXISTING SIGN TO BE MOVED
-  PROPOSED SIGN
-  EXISTING SIGN TO REMAIN

PAVEMENT MARKING LEGEND

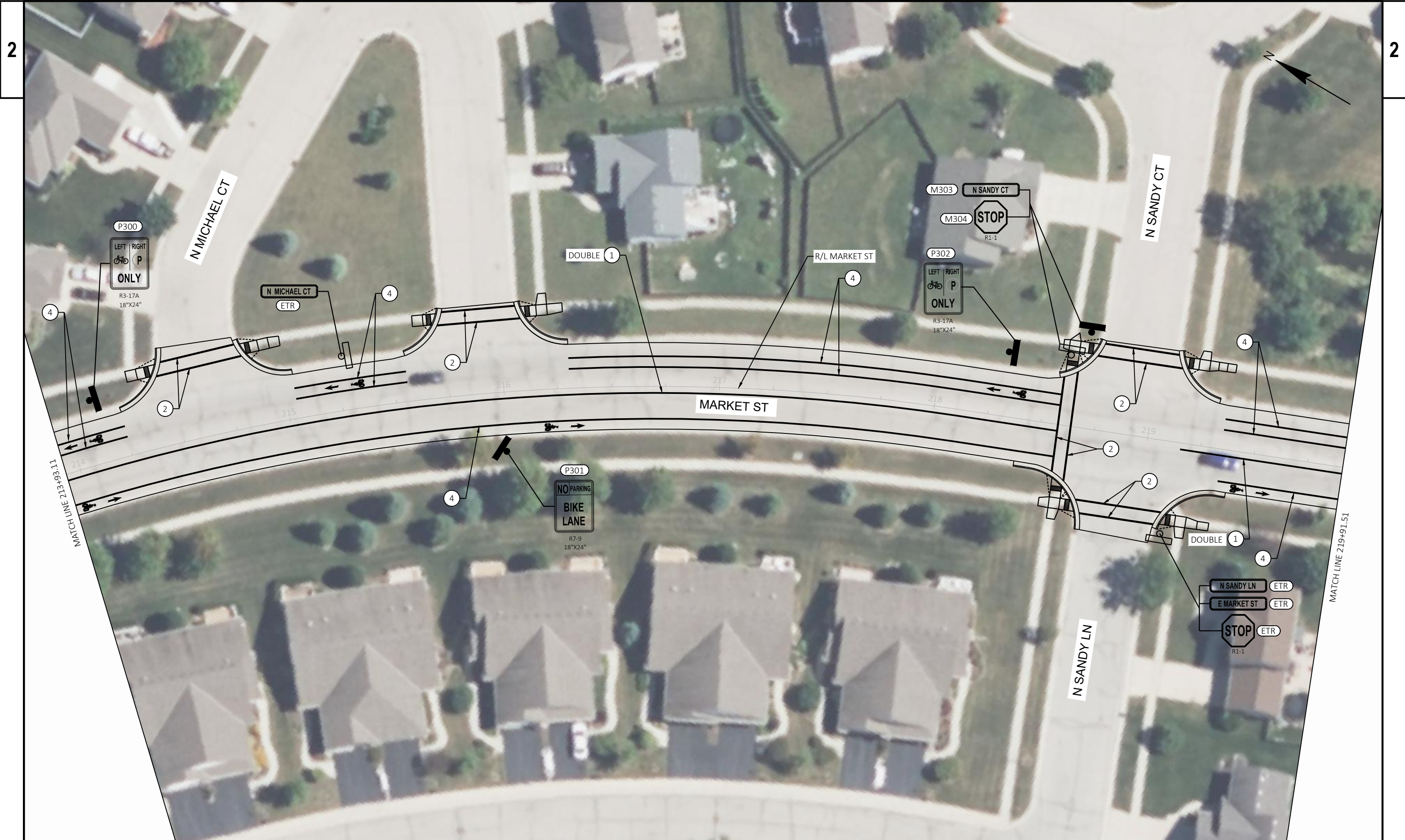
- ① MARKING LINE EPOXY 4-INCH (YELLOW)
- ② MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)
- ③ MARKING STOP LINE EPOXY 18-INCH
- ④ MARKING LINE EPOXY 4-INCH (WHITE)
- ⑤ MARKING LINE EPOXY 4-INCH (WHITE) (3' LINE, 9' GAP)
-  MARKING ARROW EPOXY - BIKE LANE ARROW
-  MARKING SYMBOL EPOXY - BIKE LANE SYMBOL

SIGNING AND PAVEMENT MARKING NOTES

- 1) REFER TO TYPICAL SECTIONS FOR LANE DIMENSIONS.
- 2) ALL CROSSWALKS ARE 6 FEET WIDE.
- 3) EXISTING SIGN POSTS WILL REMAIN FOR REPLACED SIGNS.



PROJECT NO: 3840-06-71	HWY: MARKET STREET	COUNTY: WALWORTH	SIGNING AND MARKING	SHEET	E
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2

2

PROJECT NO: 3840-06-71

HWY: MARKET STREET

COUNTY: WALWORTH

SIGNING AND MARKING

SHEET

E

FILE NAME : D:\WALWORTH_CO\ELKHORN_CITY\GOV\220543_ELKHORN MARKET ST RESURFACING BIL\34800671\SHEETSPLAN\023201-PS.DWG
LAYOUT NAME - P53

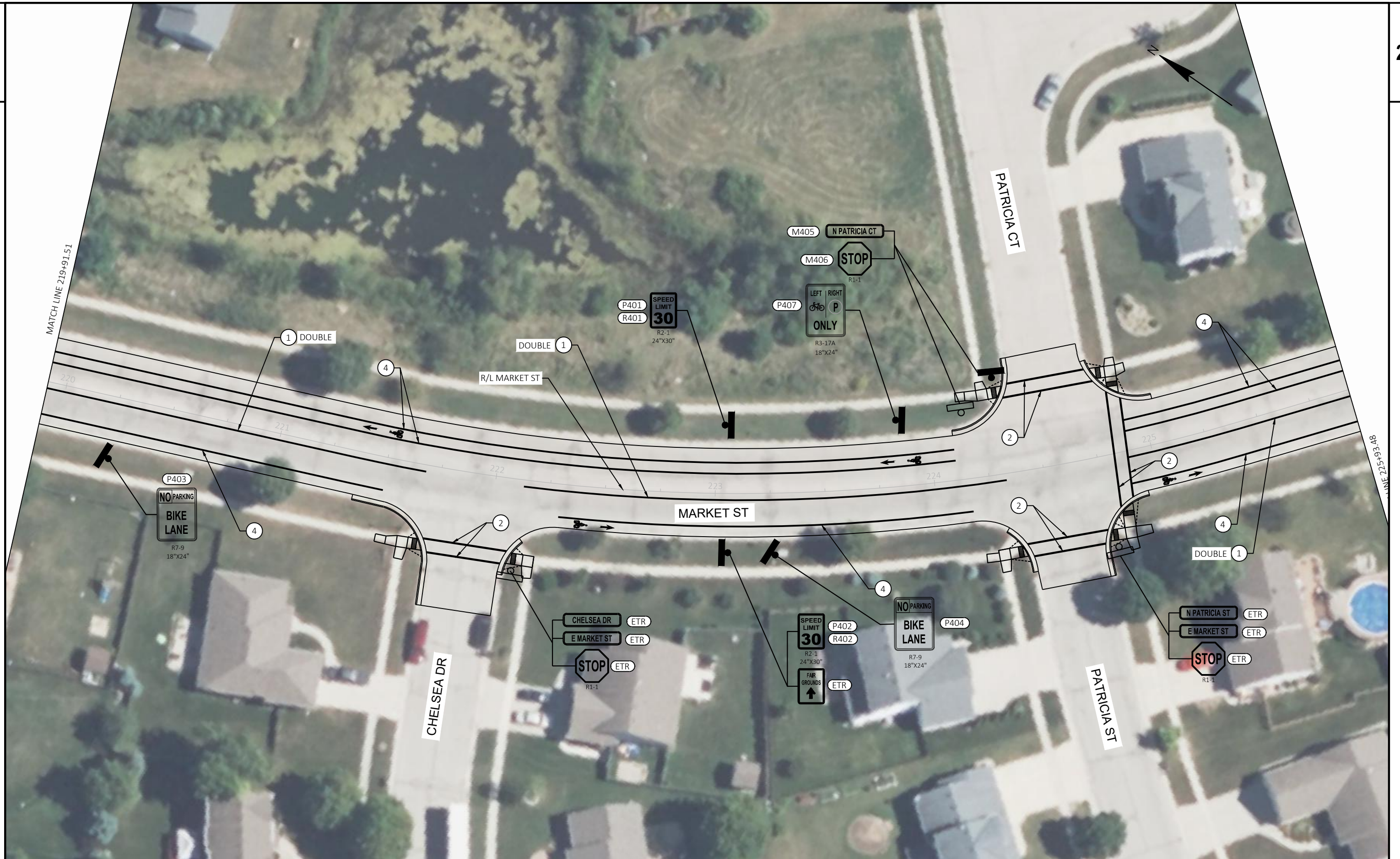
PLOT DATE : 7/28/2022 12:02 PM

PLOT BY : WALTER A. WOLAK II

PLOT NAME :

PLOT SCALE : 1 IN=40 FT

WISDOT/CADD SHEET 42



PROJECT NO: 3840-06-71

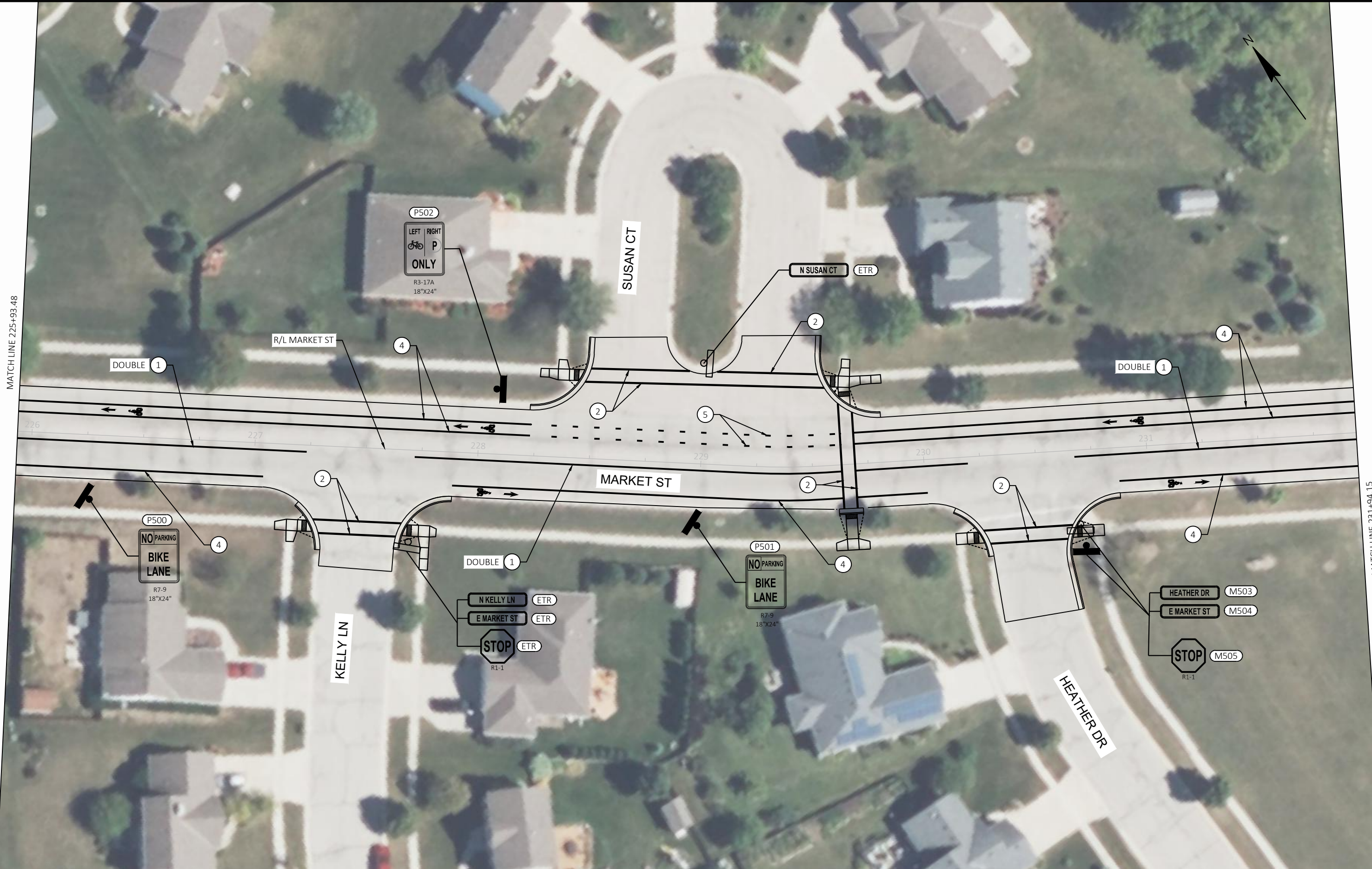
HWY: MARKET STREET

COUNTY: WALWORTH

SIGNING AND MARKING

SHEET

E





PROJECT NO: 3840-06-71

HWY: MARKET STREET

COUNTY: WALWORTH

SIGNING AND MARKING

SHEET

E



2

2

MATCH LINE 237+92.08

MATCH LINE 243+91.88

PROJECT NO: 3840-06-71	HWY: MARKET STREET	COUNTY: WALWORTH	SIGNING AND MARKING	SHEET	E
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FILE NAME : D:\WALWORTH_CO\ELKHORN_CITY\GOV\220543_ELKHORN MARKET ST RESURFACING BIL\34800671\SHEETSPLAN\023201-PS.DWG PLOT DATE : 7/28/2022 12:02 PM PLOT BY : WALTER A. WOLAK II PLOT NAME : PLOT SCALE : 1 IN:40 FT WISDOT/CADD'S SHEET 42



PROJECT NO: 3840-06-71

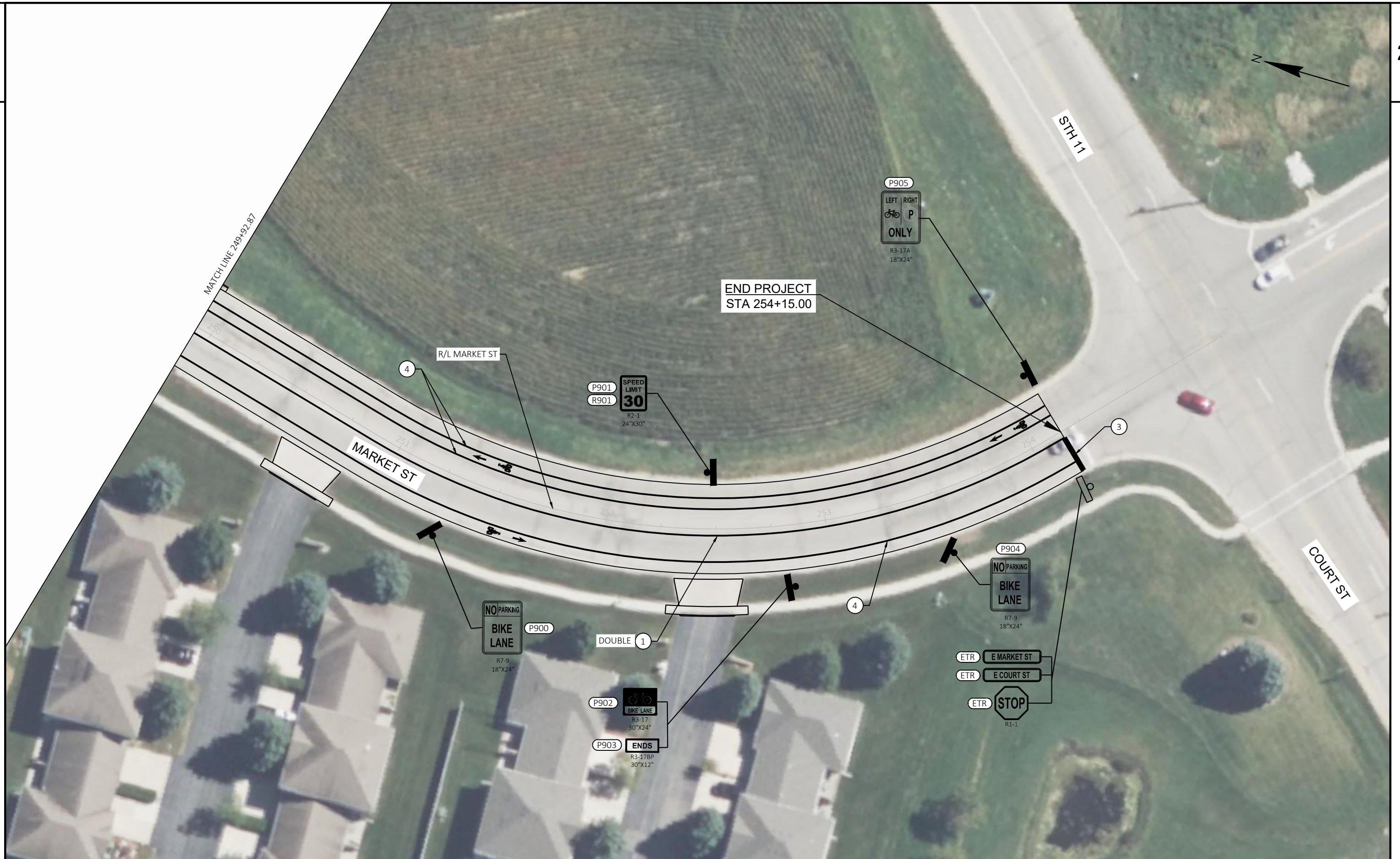
HWY: MARKET STREET

COUNTY: WALWORTH

SIGNING AND MARKING

SHEET

E



PROJECT NO: 3840-06-71

HWY: MARKET STREET

COUNTY: WALWORTH

SIGNING AND MARKING

SHEET

E

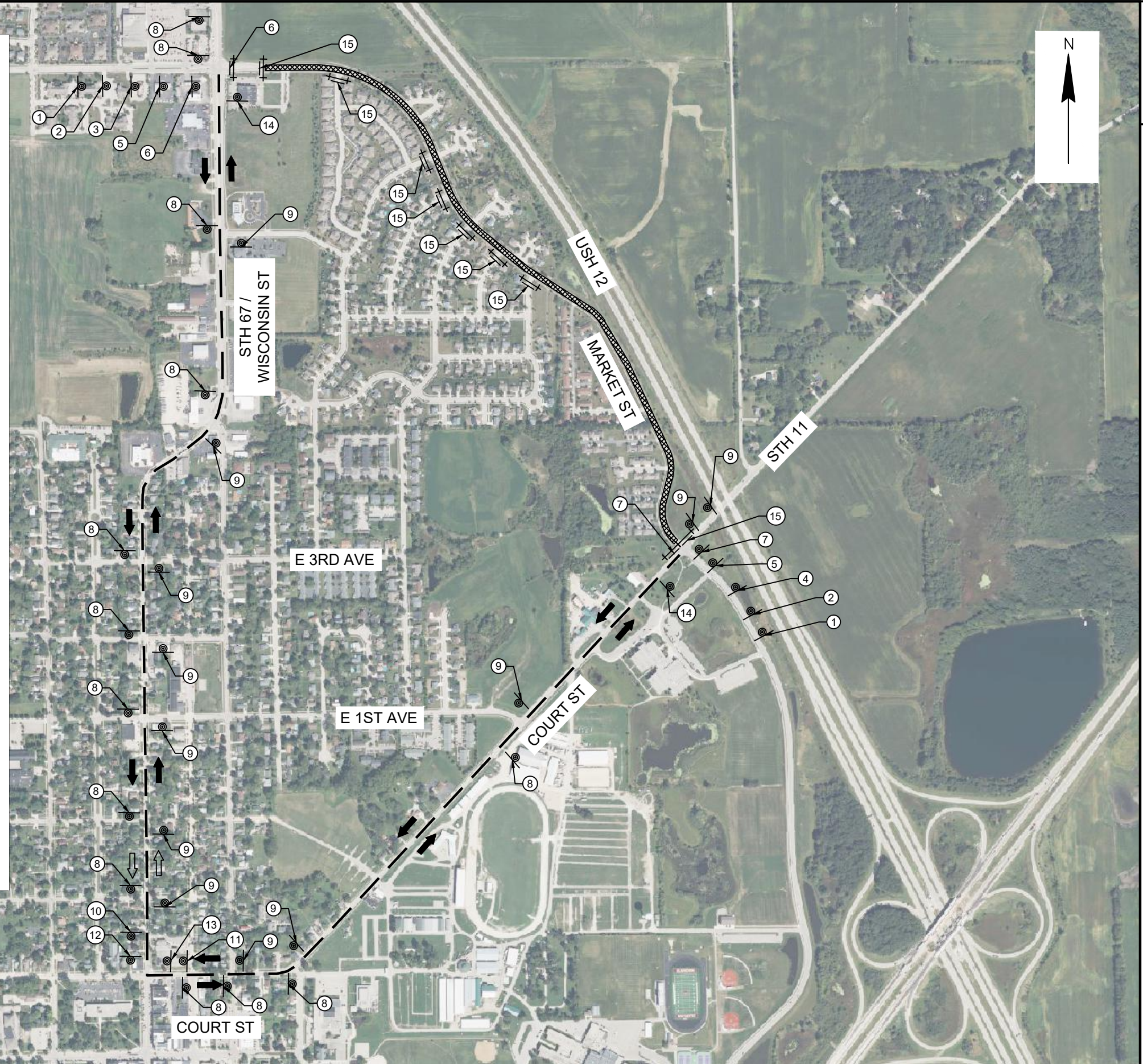
LEGEND

- TRAFFIC CONTROL SIGN WITH BARRICADE
- TRAFFIC CONTROL SIGN ON PERMANENT SUPPORT
- WORKZONE - ROAD CLOSED
- DETOUR TRAFFIC FLOW DIRECTION
- DETOUR ROUTE

① ROAD CLOSED AHEAD W20-3A	② DETOUR AHEAD W20-2A	⑩ EAST M3-2 24"x12" MARKET ST FMS (SEE CONSTRUCTION DETAILS) DETOUR M4-59L 30"x30"	⑪ WEST M3-4 24"x12" MARKET ST FMS (SEE CONSTRUCTION DETAILS) DETOUR M4-59R 30"x30"
③ DETOUR M4-59R 30"x30"	④ DETOUR M4-59L 30"x30"	⑫ EAST M3-2 24"x12" MARKET ST FMS (SEE CONSTRUCTION DETAILS) DETOUR M4-9L 30"x24"	⑬ WEST M3-4 24"x12" MARKET ST FMS (SEE CONSTRUCTION DETAILS) DETOUR M4-9R 30"x24"
⑤ ROAD CLOSED 500 FT W20-30		⑭ END DETOUR M4-8A 24"x18"	⑮ ROAD CLOSED LOCAL TRAFFIC ONLY R11-2 MOD 60"x30"
⑥ DETOUR M4-9R 30"x24"	⑦ DETOUR M4-9L 30"x24"		
⑧ DETOUR M4-8 24"x12" EAST M3-2 24"x12" MARKET ST FMS (SEE CONSTRUCTION DETAILS) M6-1 21"x21"	⑨ DETOUR M4-8 24"x12" WEST M3-4 24"x12" MARKET ST FMS (SEE CONSTRUCTION DETAILS) M6-1 21"x21"		

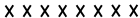






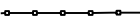




NOTE: SEE SDD'S

- BARRICADES AND SIGNS FOR MAINLINE CLOSURES
- BARRICADES AND SIGNS FOR VARIOUS CLOSURES
- BARRICADES AND SIGNS FOR SIDE ROAD CLOSURES





PEDESTRIAN STAGING LEGEND

-  CLOSED PEDESTRIAN ROUTE
-  OPEN PEDESTRIAN ROUTE
-  WORK ZONE THIS STAGE
-  WORK COMPLETED IN PREVIOUS STAGE
-  TEMPORARY PEDESTRIAN CURB RAMP
-  TEMPORARY PEDESTRIAN DETECTABLE WARNING FIELD
-  TEMPORARY PEDESTRIAN SURFACE
-  TEMPORARY PEDESTRIAN BARRICADE
-  TEMPORARY MARKING LINE, REMOVABLE TAPE 4-INCH (WHITE)
-  TYPE II BARRICADE WITH ATTACHED SIGN
-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL DRUM



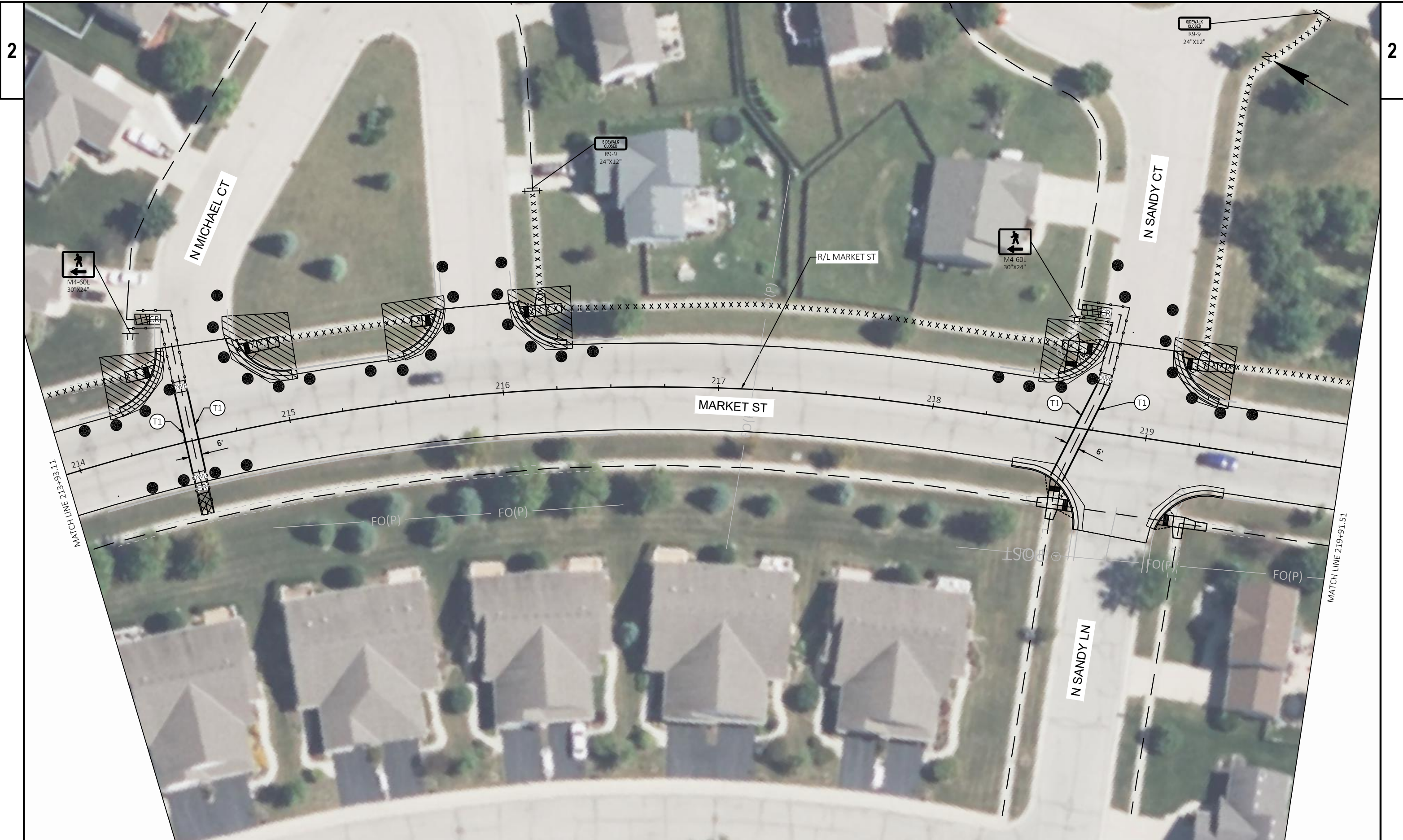
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2

PROJECT NO: 3840-06-71	HWY: MARKET STREET	COUNTY: WALWORTH	PEDESTRIAN STAGING PLAN STAGE 1	SHEET	E
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FILE NAME : D:\WALWORTH_CO\ELKHORN_CITY\GOV\220543_ELKHORN MARKET ST RESURFACING BIL\34800671\SHEETSPLAN\027011-DT.DWG PLOT DATE : 8/30/2022 4:12 PM PLOT BY : NICHOLAS BOBINSKI, PE PLOT NAME : PLOT SCALE : 1 IN:40 FT WISDOT/CADD SHEET 42

LAYOUT NAME - PD-DET2



PROJECT NO: 3840-06-71

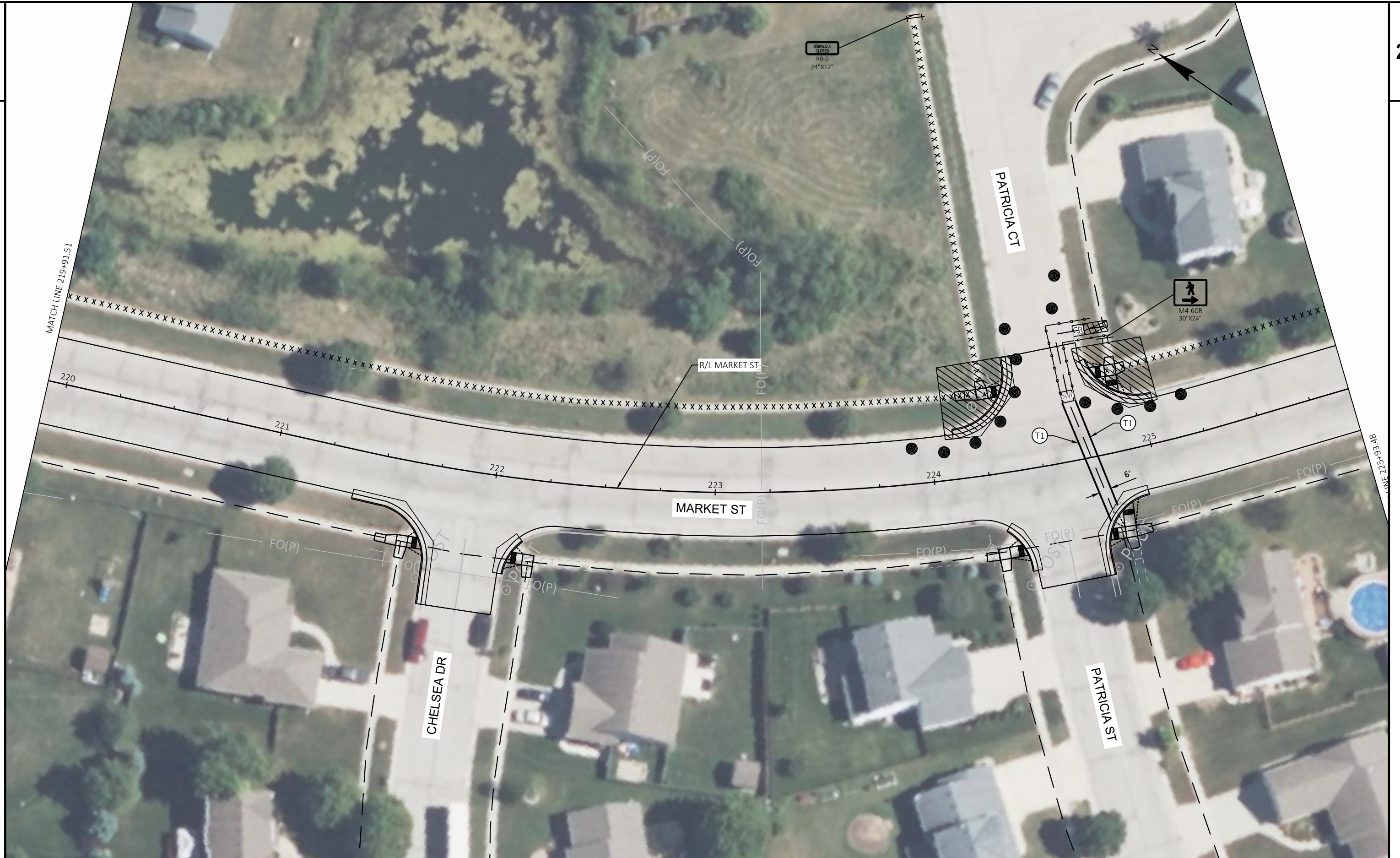
HWY: MARKET STREET

COUNTY: WALWORTH

PEDESTRIAN STAGING PLAN STAGE 1

SHEET

E



PROJECT NO: 3840-06-71

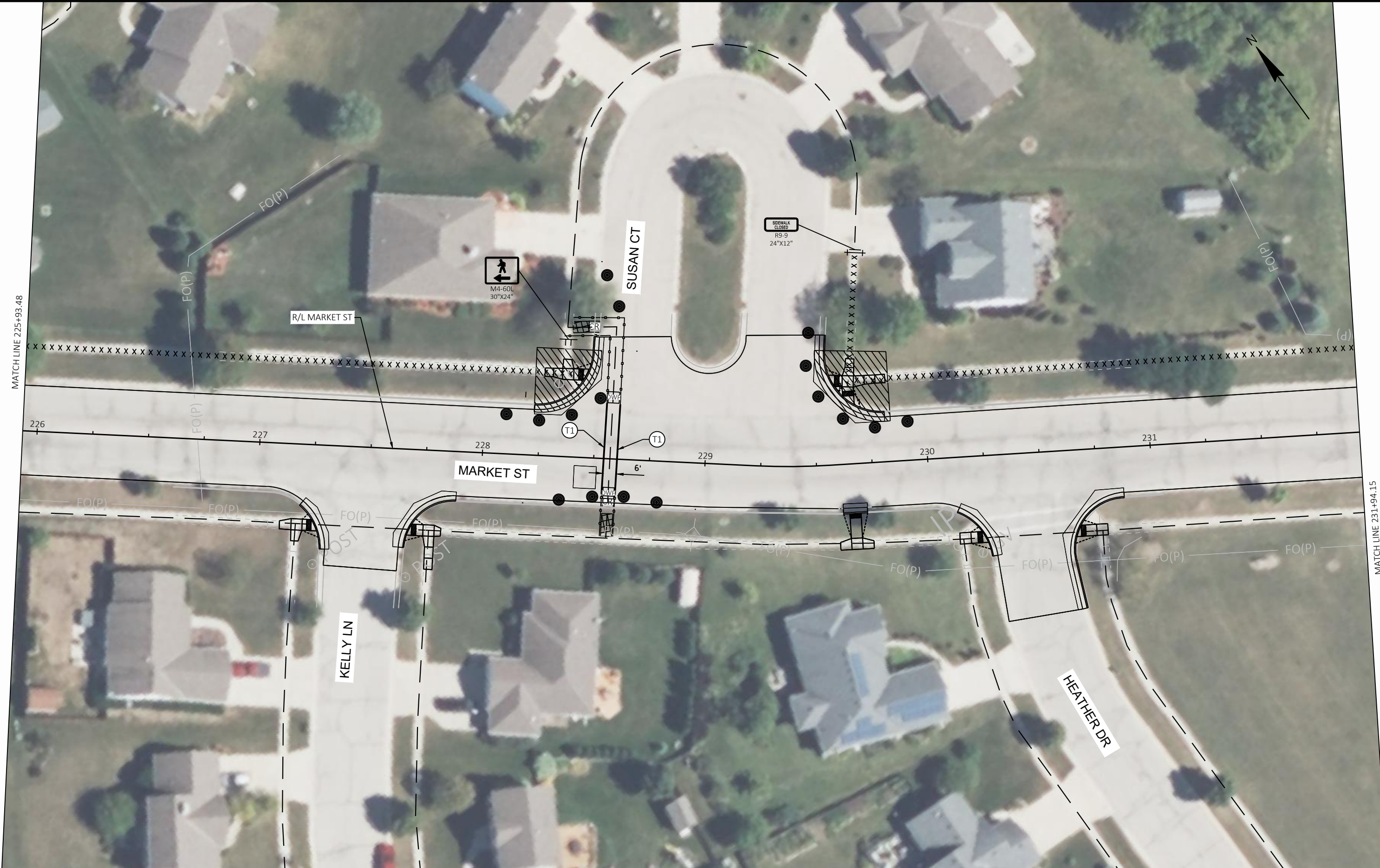
HWY: MARKET STREET

COUNTY: WALWORTH

PEDESTRIAN STAGING PLAN STAGE 1

SHEET

E



PROJECT NO: 3840-06-71

HWY: MARKET STREET

COUNTY: WALWORTH

PEDESTRIAN STAGING PLAN STAGE 1

SHEET

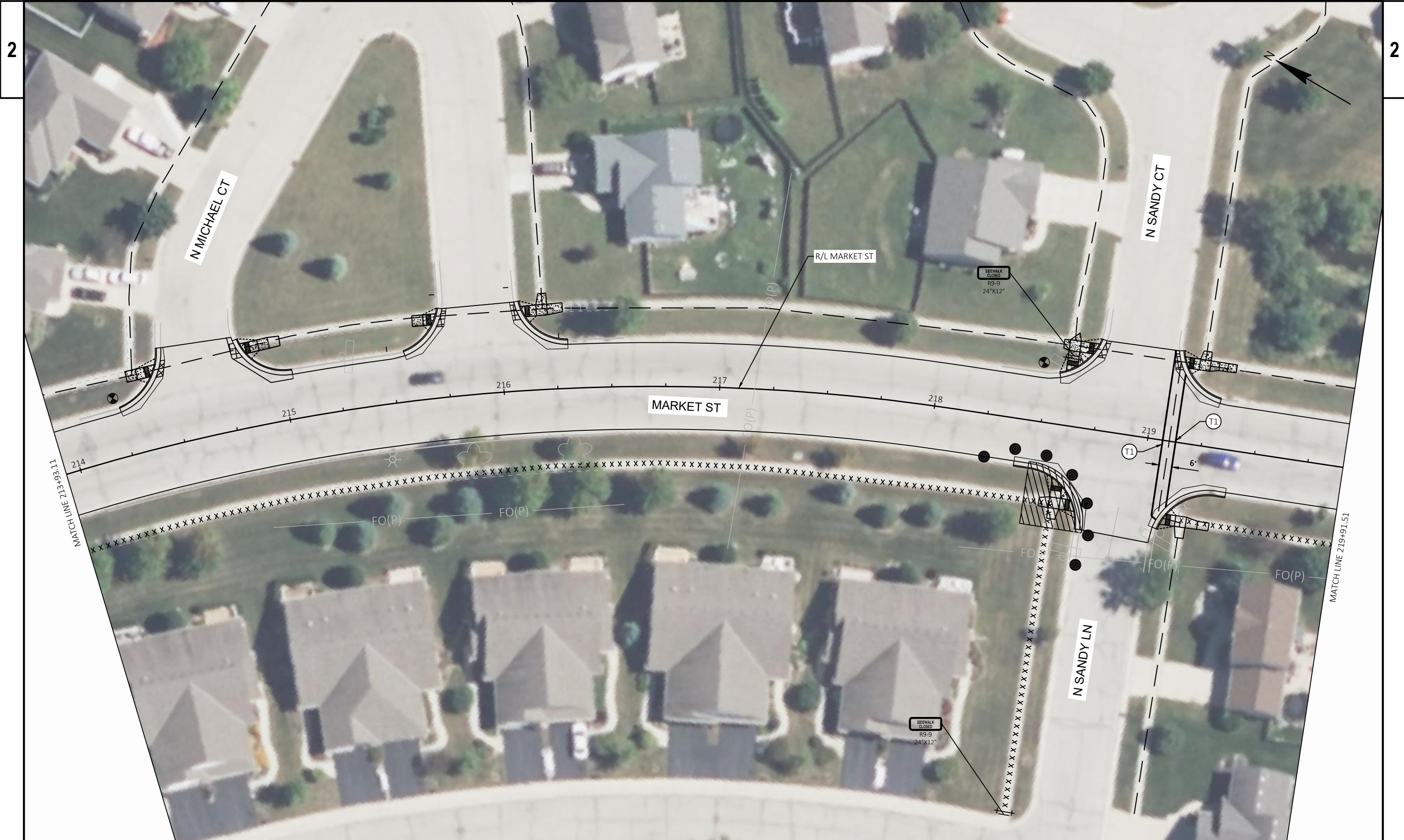
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2

PROJECT NO: 3840-06-71	HWY: MARKET STREET	COUNTY: WALWORTH	PEDESTRIAN STAGING PLAN STAGE 2	SHEET	E
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2

2

PROJECT NO: 3840-06-71

HWY: MARKET STREET

COUNTY: WALWORTH

PEDESTRIAN STAGING PLAN STAGE 2

SHEET

E

FILE NAME : D:\WALWORTH_CO\ELKHORN_CITY\GOV\220543_ELKHORN MARKET ST RESURFACING BIL\34800671\SHEETSPLAN\027021-DT.DWG
LAYOUT NAME - PD-DET3

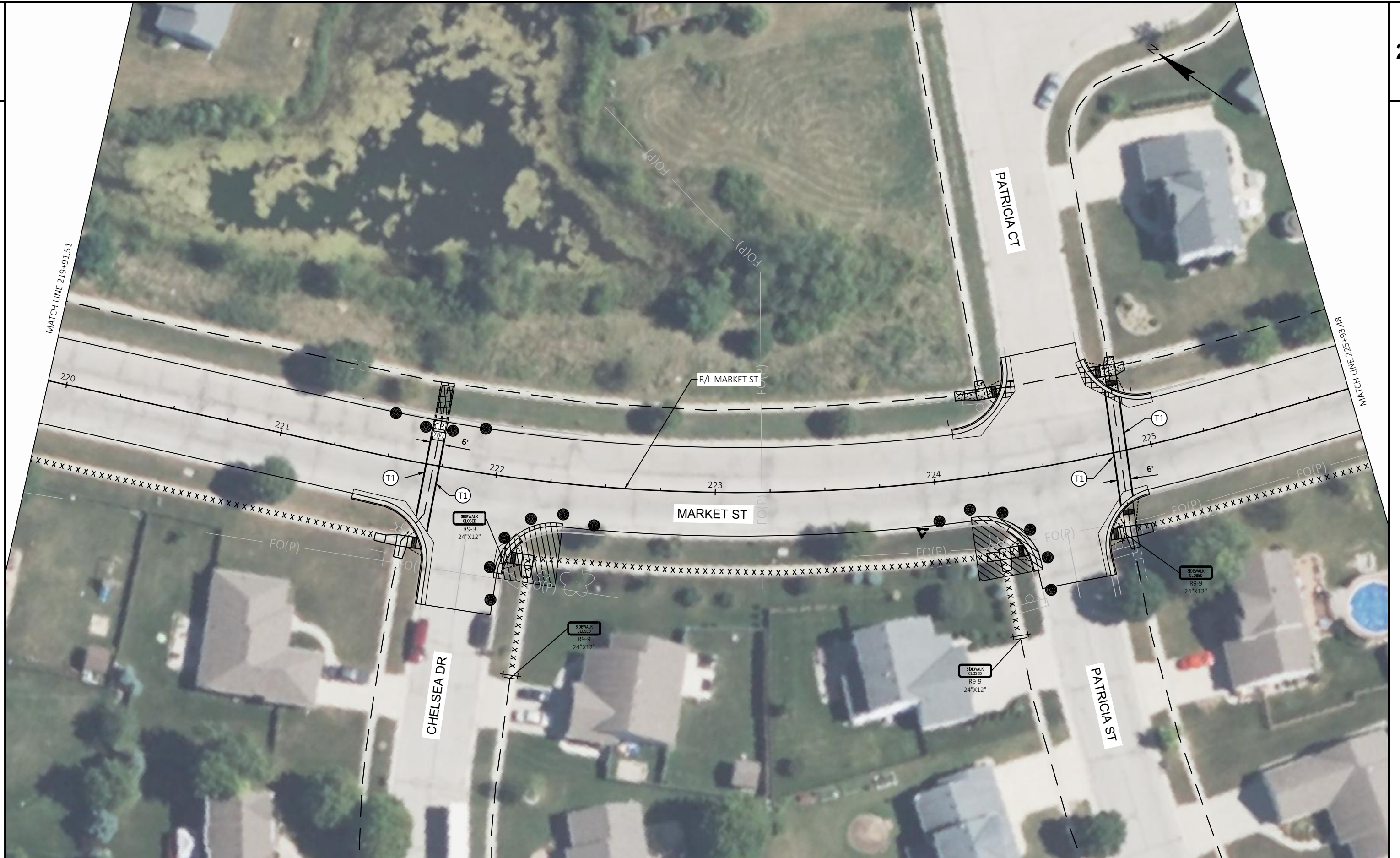
PLOT DATE : 8/30/2022 4:13 PM

PLOT BY : NICHOLAS BOBINSKI, PE

PLOT NAME :

PLOT SCALE : 1 IN:40 FT

WISDOT/CADDs SHEET 42



PROJECT NO: 3840-06-71

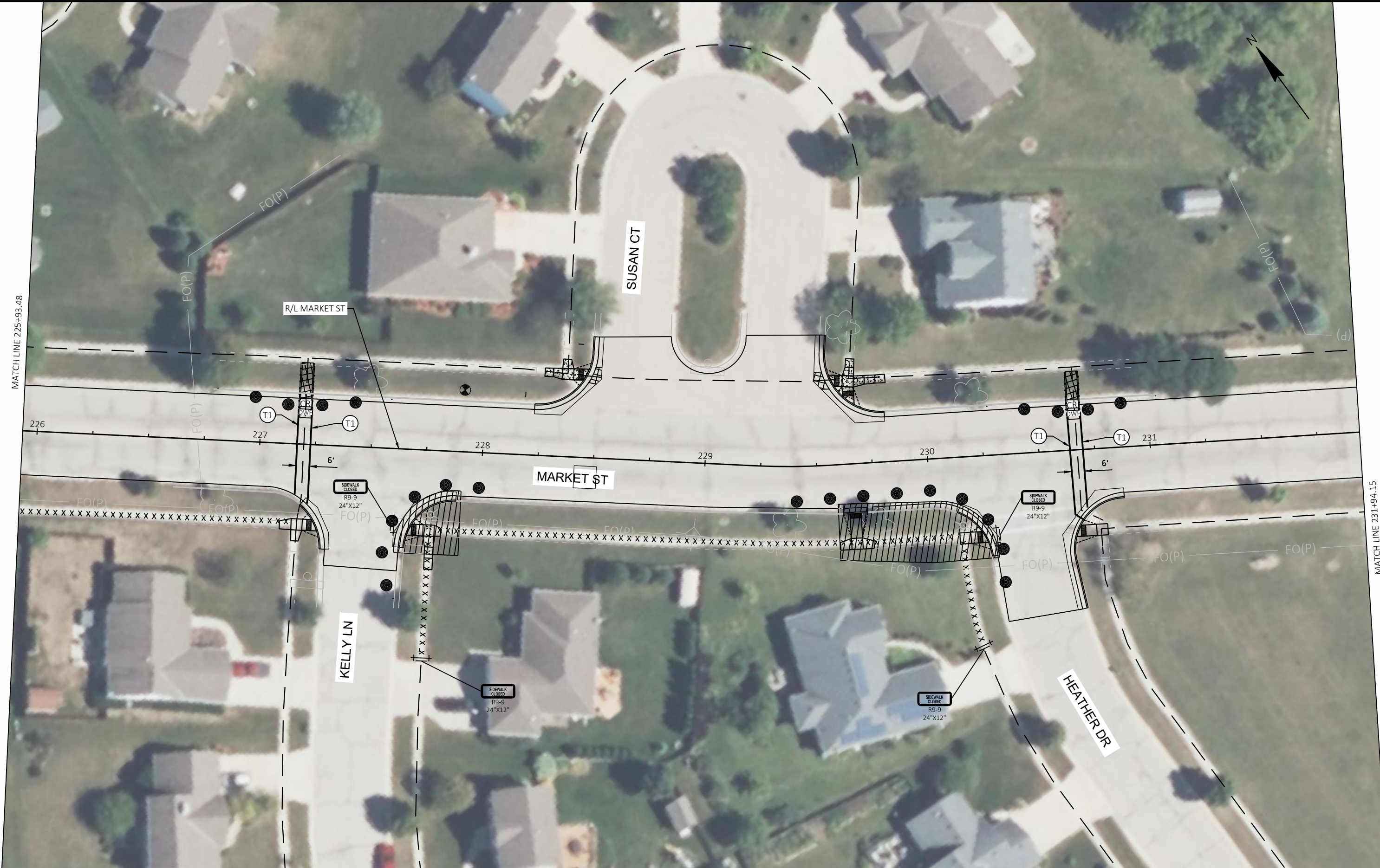
HWY: MARKET STREET

COUNTY: WALWORTH

PEDESTRIAN STAGING PLAN STAGE 2

SHEET

E



PROJECT NO: 3840-06-71

HWY: MARKET STREET

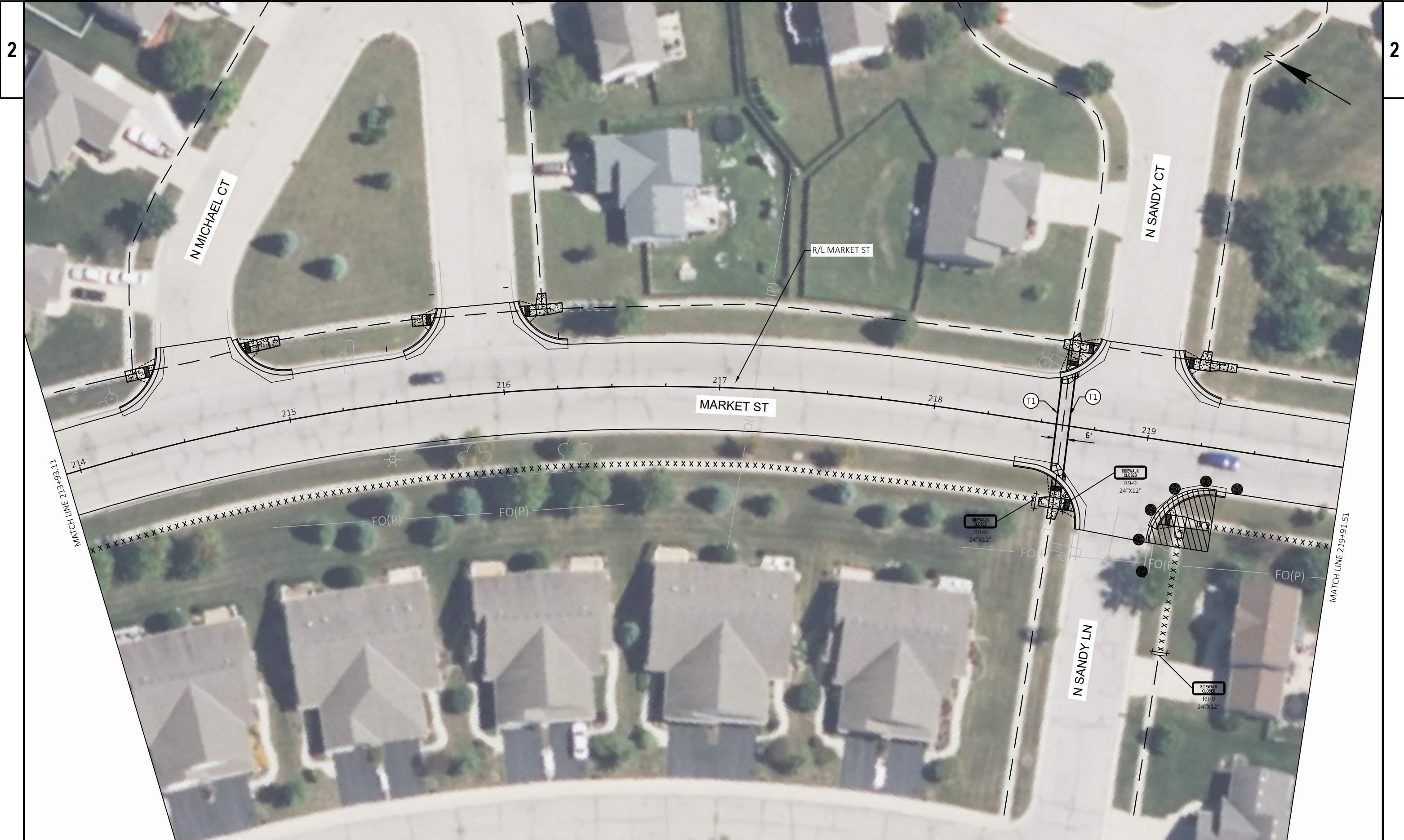
COUNTY: WALWORTH

PEDESTRIAN STAGING PLAN STAGE 2

SHEET

E





2

2

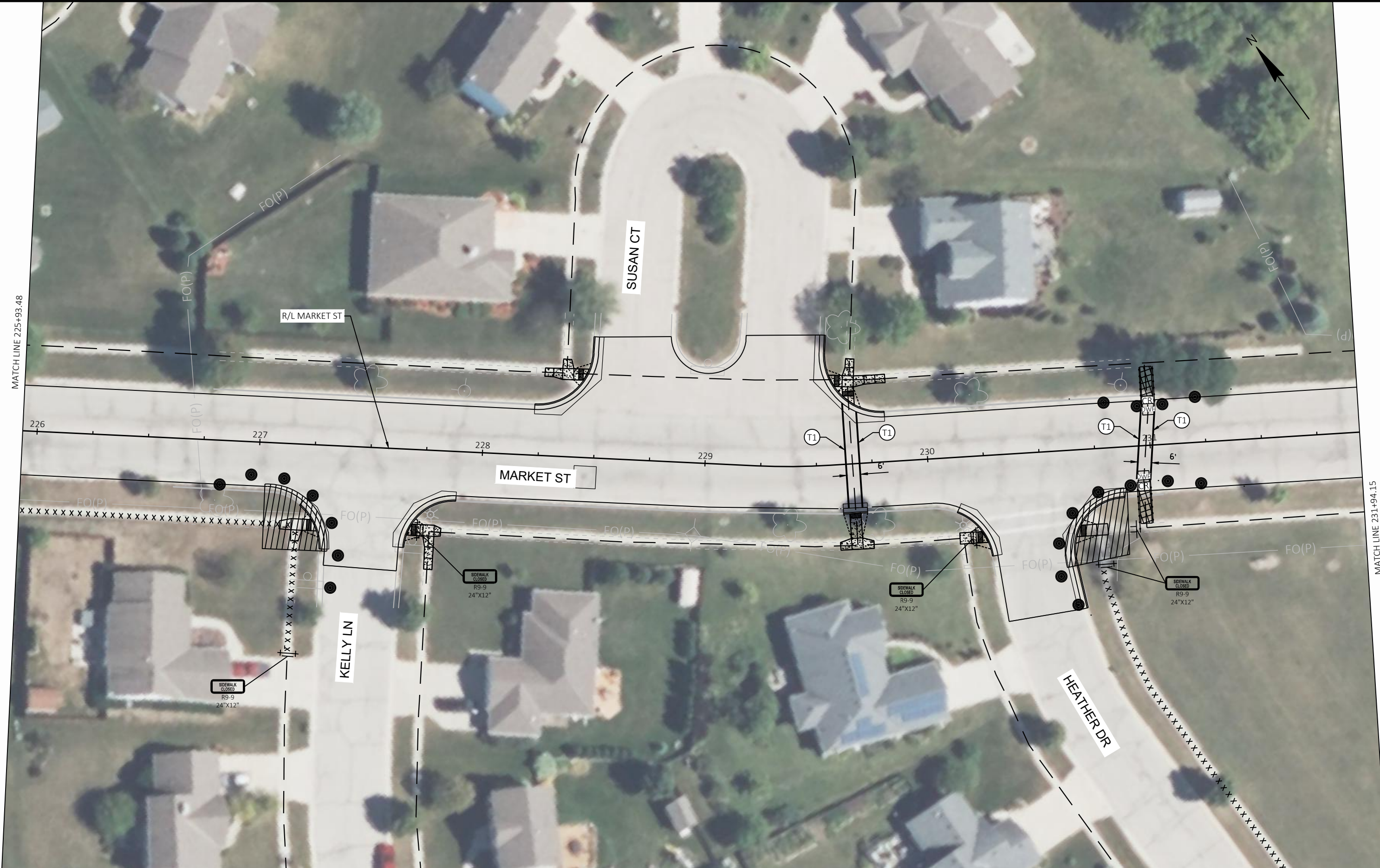
PROJECT NO: 3840-06-71	HWY: MARKET STREET	COUNTY: WALWORTH	PEDESTRIAN STAGING PLAN STAGE 3	SHEET	E
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LAYOUT NAME - PD-DET3



PROJECT NO: 3840-06-71	HWY: MARKET STREET	COUNTY: WALWORTH	PEDESTRIAN STAGING PLAN STAGE 3	SHEET E
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PROJECT NO: 3840-06-71

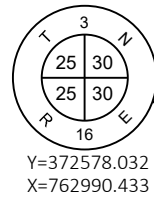
HWY: MARKET STREET

COUNTY: WALWORTH

PEDESTRIAN STAGING PLAN STAGE 3

SHEET

E



BEGIN PROJECT
STA 203+50.00
 Y=372579.767
 X=763354.150

CP200
 Y = 372552.241
 X = 763180.443
 EL = 995.48
 CMK

BM 602222067
 Y = 372547.231
 X = 763344.029
 EL = 998.92
 BM RR SPIKE SOUTH S POLE

BM 602222069
 Y = 372550.980
 X = 763348.994
 EL = 1000.35
 BM TOP NUT HYDRANT

CP 2001
 Y = 372558.459
 X = 763431.857
 EL = 998.14
 CMK

**STH 67 /
 WISCONSIN ST**

MARKET ST

SWEETBRIAR DR

N SANDY LN

CHELSEA DR

PATRICIA ST

KELLY LN

HEATHER DR

DYLAN DR

ALEX DR

STH 11

COURT ST

END PROJECT
STA 254+15.00
 Y=369107.953
 X=766362.982

<p>CURVE 1 PI STA = 213+30.03 Y = 372585.781 X = 764334.157 DELTA = 68°17'46" RT D = 6°08'04" T = 633.51' L = 1113.32' R = 934.00' PC STA = 206+96.52 Y = 372581.894 X = 763700.660 PT STA = 218+09.84 Y = 371998.632 X = 764572.042 DB = N89°38'54"E DA = S22°03'20"E</p>	<p>CURVE 2 PI STA = 221+35.95 Y = 371696.387 X = 764694.498 PT STA = 225+40.52 Y = 371374.799 X = 764932.837 DB = S22°03'20"E DA = S51°01'52"E</p>	<p>CURVE 3 PI STA = 229+39.62 Y = 371123.811 X = 765243.127 DELTA = 6°18'36" LT D = 19°05'55" T = 16.54' L = 33.04' R = 300.00' PC STA = 229+23.08 Y = 371134.210 X = 765230.271 PT STA = 229+56.12 Y = 371114.887 X = 765257.049 DB = S51°01'52"E DA = S57°20'28"E</p>	<p>CURVE 4 PI STA = 235+89.52 Y = 370773.084 X = 765790.303 DELTA = 26°48'44" RT D = 19°05'55" T = 71.50' L = 140.39' R = 300.00' PC STA = 235+18.01 Y = 370811.670 X = 765730.104 PT STA = 236+58.40 Y = 370711.493 X = 765826.625 DB = S57°20'28"E DA = S30°31'44"E</p>
<p>CURVE 5 PI STA = 240+13.15 Y = 370405.917 X = 766006.830 DELTA = 5°39'05" RT D = 19°05'55" T = 14.81' L = 29.59' R = 300.00' PC STA = 239+98.35 Y = 370418.671 X = 765999.309 PT STA = 240+27.94 Y = 370392.483 X = 766013.059 DB = S30°31'44"E DA = S24°52'39"E</p>	<p>CURVE 6 PI STA = 248+16.81 Y = 369676.808 X = 766344.923 DELTA = 39°31'25" RT D = 19°05'55" T = 107.78' L = 206.94' R = 300.00' PC STA = 247+09.03 Y = 369774.587 X = 766299.582 PT STA = 249+15.98 Y = 369572.530 X = 766317.671 DB = S24°52'39"E DA = S14°38'46"W</p>	<p>CURVE 7 PI STA = 252+67.37 Y = 369232.557 X = 766228.823 DELTA = 61°44'31" LT D = 19°05'55" T = 179.34' L = 323.28' R = 300.00' PC STA = 250+88.03 Y = 369406.070 X = 766274.168 PT STA = 254+11.31 Y = 369110.467 X = 766360.189 DB = S14°38'46"W DA = S47°05'46"E</p>	

Estimate Of Quantities

3840-06-71

Line	Item	Item Description	Unit	Total	Qty
0002	204.0100	Removing Concrete Pavement	SY	288.000	288.000
0004	204.0115	Removing Asphaltic Surface Butt Joints	SY	320.000	320.000
0006	204.0120	Removing Asphaltic Surface Milling	SY	24,531.000	24,531.000
0008	204.0150	Removing Curb & Gutter	LF	885.000	885.000
0010	204.0155	Removing Concrete Sidewalk	SY	247.000	247.000
0012	205.0100	Excavation Common	CY	4,149.000	4,149.000
0014	211.0101	Prepare Foundation for Asphaltic Paving (project) 01. 3840-06-71	EACH	1.000	1.000
0016	213.0100	Finishing Roadway (project) 01. 3840-06-71	EACH	1.000	1.000
0018	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	9,516.000	9,516.000
0020	416.0160	Concrete Driveway 6-Inch	SY	175.000	175.000
0022	416.0260	Concrete Driveway HES 6-Inch	SY	114.000	114.000
0024	455.0605	Tack Coat	GAL	1,806.000	1,806.000
0026	460.2000	Incentive Density HMA Pavement	DOL	3,780.000	3,780.000
0028	460.6223	HMA Pavement 3 MT 58-28 S	TON	3,317.000	3,317.000
0030	460.6224	HMA Pavement 4 MT 58-28 S	TON	2,581.000	2,581.000
0032	465.0120	Asphaltic Surface Driveways and Field Entrances	TON	2.000	2.000
0034	601.0411	Concrete Curb & Gutter 30-Inch Type D	LF	885.000	885.000
0036	602.0410	Concrete Sidewalk 5-Inch	SF	2,691.000	2,691.000
0038	602.0505	Curb Ramp Detectable Warning Field Yellow	SF	280.000	280.000
0040	602.0605	Curb Ramp Detectable Warning Field Radial Yellow	SF	24.000	24.000
0042	611.8110	Adjusting Manhole Covers	EACH	2.000	2.000
0044	611.8115	Adjusting Inlet Covers	EACH	8.000	8.000
0046	619.1000	Mobilization	EACH	1.000	1.000
0048	623.0200	Dust Control Surface Treatment	SY	24,531.000	24,531.000
0050	624.0100	Water	MGAL	34.000	34.000
0052	625.0100	Topsoil	SY	1,292.000	1,292.000
0054	627.0200	Mulching	SY	1,292.000	1,292.000
0056	628.1905	Mobilizations Erosion Control	EACH	1.000	1.000
0058	628.1910	Mobilizations Emergency Erosion Control	EACH	4.000	4.000
0060	628.7020	Inlet Protection Type D	EACH	48.000	48.000
0062	629.0210	Fertilizer Type B	CWT	1.000	1.000
0064	630.0200	Seeding Temporary	LB	35.000	35.000
0066	631.0300	Sod Water	MGAL	261.000	261.000
0068	631.1000	Sod Lawn	SY	1,292.000	1,292.000
0070	634.0814	Posts Tubular Steel 2x2-Inch X 14-FT	EACH	30.000	30.000
0072	637.2210	Signs Type II Reflective H	SF	129.000	129.000
0074	638.2102	Moving Signs Type II	EACH	7.000	7.000
0076	638.2602	Removing Signs Type II	EACH	6.000	6.000
0078	638.4000	Moving Small Sign Supports	EACH	5.000	5.000
0080	642.5201	Field Office Type C	EACH	1.000	1.000
0082	643.0300	Traffic Control Drums	DAY	2,599.000	2,599.000
0084	643.0420	Traffic Control Barricades Type III	DAY	2,018.000	2,018.000
0086	643.0900	Traffic Control Signs	DAY	10,229.000	10,229.000
0088	643.0920	Traffic Control Covering Signs Type II	EACH	4.000	4.000
0090	643.1000	Traffic Control Signs Fixed Message	SF	87.000	87.000
0092	643.1050	Traffic Control Signs PCMS	DAY	50.000	50.000
0094	643.3105	Temporary Marking Line Paint 4-Inch	LF	1,379.000	1,379.000
0096	643.5000	Traffic Control	EACH	1.000	1.000
0098	644.1410	Temporary Pedestrian Surface Asphalt	SF	1,804.000	1,804.000

Estimate Of Quantities

3840-06-71

Line	Item	Item Description	Unit	Total	Qty
0100	644.1601	Temporary Pedestrian Curb Ramp	DAY	234.000	234.000
0102	644.1605	Temporary Pedestrian Detectable Warning Field	SF	204.000	204.000
0104	644.1810	Temporary Pedestrian Barricade	LF	1,188.000	1,188.000
0106	645.0220	Geogrid Type SR	SY	12,266.000	12,266.000
0108	646.1020	Marking Line Epoxy 4-Inch	LF	23,831.000	23,831.000
0110	646.5020	Marking Arrow Epoxy	EACH	39.000	39.000
0112	646.5220	Marking Symbol Epoxy	EACH	39.000	39.000
0114	646.6120	Marking Stop Line Epoxy 18-Inch	LF	17.000	17.000
0116	646.7420	Marking Crosswalk Epoxy Transverse Line 6-Inch	LF	1,284.000	1,284.000
0118	690.0150	Sawing Asphalt	LF	1,835.000	1,835.000
0120	690.0250	Sawing Concrete	LF	352.000	352.000
0122	ASP.1T0A	On-the-Job Training Apprentice at \$5.00/HR	HRS	500.000	500.000
0124	ASP.1T0G	On-the-Job Training Graduate at \$5.00/HR	HRS	900.000	900.000
0126	SPV.0060	Special 01. Survey Project ID 3840-06-71	EACH	1.000	1.000
0128	SPV.0060	Special 02. Curb Ramp Grading, Shaping, and Finishing	EACH	30.000	30.000
0130	SPV.0060	Special 51. Adjusting Sanitary Manhole	EACH	5.000	5.000
0132	SPV.0060	Special 52. Adjusting Water Valve Boxes	EACH	5.000	5.000
0134	SPV.0075	Special 01. Pavement Cleanup Project	HRS	16.000	16.000

3

REMOVALS

CATEGORY	ROADWAY	STATION	TO	STATION	204.0100	204.0120	204.0150	204.0155
					REMOVING CONCRETE PAVEMENT SY	REMOVING ASPHALTIC SURFACE MILLING SY	REMOVING CURB & GUTTER LF	REMOVING CONCRETE SIDEWALK SY
0010	MARKET STREET	203+50	-	254+15	288	24,531	885	247
TOTAL 0010					288	24,531	885	247

AGGREGATE ITEMS

CATEGORY	ROADWAY	STATION	TO	STATION	305.0120
					BASE AGGREGATE DENSE 1 1/4- INCH TON
0010	MARKET ST EBS*	203+50	-	254+15	9,405
0010	DRIVEWAYS	243+40	-	252+65	111
TOTAL 0010					9,516

*EBS TO BE BACKFILLED WITH BASE AGGREGATE DENSE 1 1/4-INCH AT LOCATIONS DETERMINED BY THE ENGINEER.
SEE CONSTRUCTION DETAIL.

PREPARE FOUNDATION FOR ASPHALTIC SURFACE

CATEGORY	LOCATION	211.0101.01
		PREPARE FOUNDATION FOR ASPHALTIC PAVING (3840-06-71) EACH
0010	MARKET ST	1
TOTAL 0010		1

FINISHING ROADWAY

CATEGORY	PROJECT	213.0100.01
		FINISHING ROADWAY (3840-06-71) EACH
0010	PROJECT 3840-06-71	1
TOTAL 0010		1

BUTT JOINTS

CATEGORY	ROADWAY	204.0115
		REMOVING ASPHALTIC SURFACE BUTT JOINTS SY
0010	MARKET ST BEGIN	27
0010	SWEETBRIAR DR	17
0010	N MICHAEL CT NORTH	23
0010	N MICHAEL CT SOUTH	24
0010	N SANDY CT	24
0010	N SANDY LN	22
0010	CHELSEA DR	22
0010	PATRICIA CT	22
0010	PATRICIA ST	22
0010	KELLY LN	22
0010	SUSAN CT WEST	22
0010	SUSAN CT EAST	23
0010	HEATHER DR	23
0010	MARKET ST END	27
TOTAL 0010		320

EXCAVATION COMMON

CATEGORY	ROADWAY	STATION	TO	STATION	205.0100
					EXCAVATION COMMON CY
0010	MARKET ST EBS*	203+50	-	254+15	4,089
0010	DRIVEWAYS	243+40	-	252+65	24
0010	TERRACE	243+40	-	254+15	36
TOTAL 0010					4,149

*EBS TO BE CONDUCTED AT LOCATIONS DETERMINED BY THE ENGINEER.
EBS VOLUME ASSUMED AS 50% OF TOTAL MILLING AREA AT 12" DEPTH.
SEE CONSTRUCTION DETAIL.

3

3

DRIVEWAY CONCRETE

CATEGORY	LOCATION	416.0160 CONCRETE DRIVEWAY 6- INCH SY	416.0260 CONCRETE DRIVEWAY HES 6- INCH SY
0010	MARKET ST	175	114
	TOTAL 0010	175	114

HMA PAVEMENT

CATEGORY	ROADWAY	STATION	TO	STATION	TACK COAT GAL	HMA PAVEMENT 3 MT 58-28 S TON	HMA PAVEMENT 4 MT 58-28 S TON	465.0120 ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES TON
0010	MARKET ST	203+50	-	208+00	148	272	211	-
0010	MARKET ST	208+00	-	214+00	202	371	289	-
0010	MARKET ST	214+00	-	220+00	232	426	331	-
0010	MARKET ST	220+00	-	226+00	234	429	334	-
0010	MARKET ST	226+00	-	232+00	255	468	364	-
0010	MARKET ST	232+00	-	238+00	199	366	285	-
0010	MARKET ST	238+00	-	244+00	199	366	285	-
0010	MARKET ST	244+00	-	250+00	199	366	285	-
0010	MARKET ST	250+00	-	254+15	138	253	197	2
	TOTAL 0010				1,806	3,317	2,581	2

3

CURB & GUTTER

CATEGORY	ROADWAY	601.0411 CONCRETE CURB & GUTTER 30- INCH TYPED LF
0010	SWEETBRIAR DR	81
0010	MICHAEL CT	125
0010	SANDY LN/SANDY CT	142
0010	CHELSEA DR	86
0010	PATRICIA CT	83
0010	PATRICIA ST	68
0010	KELLY LN	58
0010	SUSAN CT	97
0010	HEATHER DR	90
0010	MARKET ST INLET REPAIRS	55
	TOTAL 0010	885

CONCRETE SIDEWALK

CATEGORY	LOCATION	602.0410 CONCRETE SIDEWALK 5- INCH SF	602.0505 CURB RAMP DETECTABLE YELLOW SF	602.0605 CURB RAMP DETECTABLE RADIAL YELLOW SF
0010	SWEETBRIAR DR	450	20	24
0010	MICHAEL CT	310	40	-
0010	SANDY LN/SANDY CT	499	60	-
0010	CHELSEA DR	195	20	-
0010	PATRICIA CT	227	30	-
0010	PATRICIA ST	266	30	-
0010	KELLY LN	225	20	-
0010	SUSAN CT	395	40	-
0010	HEATHER DR	124	20	-
	TOTAL 0010	2,691	280	24

ADJUSTING MANHOLES AND INLETS

CATEGORY	LOCATION	611.8110 ADJUSTING MANHOLE COVERS EACH	611.8115 ADJUSTING INLET COVERS EACH
0010	MARKET ST	2	8
	TOTAL 0010	2	8

DUST CONTROL

CATEGORY	PROJECT	623.0200 DUST CONTROL SURFACE TREATMENT SY
0010	PROJECT 3840-06-71	24,531
	TOTAL 0010	24,531

MOBILIZATION

CATEGORY	PROJECT	619.1000 MOBILIZATION EACH
0010	3840-06-71	1
	TOTAL 0010	1

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3

EROSION CONTROL & RESTORATION

CATEGORY	LOCATION	STATION	TO	STATION	OFFSET	625.0100	627.0200	628.1905	628.1910	628.7020	629.0210	630.0200	631.0300	631.1000
						TOPSOIL SY	MULCHING SY	MOBILIZATIONS EROSION CONTROL EACH	MOBILIZATIONS EMERGENCY EROSION CONTROL EACH	INLET PROTECTION TYPE D EACH	FERTILIZER TYPE B CWT	SEEDING TEMPORARY LB	SOD WATER MGAL	SOD LAWN SY
0010	MARKET ST	203+50	-	254+15		--	--	1	4	44	--	--	--	--
0010	TERRACE	243+40	-	254+15		1,292	1,292	--	--	--	1	35	261	1,292
SUBTOTAL						1,292	1,292	1	4	44	1	35	261	1,292
UNDISTRIBUTED						--	--	--	--	4	--	--	--	--
TOTAL 0010						1,292	1,292	1	4	48	1	35	261	1,292

*SEEDING TEMPORARY AND MULCHING ITEMS TO BE USED AT THE DIRECTION OF THE ENGINEER PER THE EROSION CONTROL ARTICLE IN THE SPECIAL PROVISIONS

WATER

LOCATION	624.0100 WATER MGAL
PROJECT-BASE AGGREGATE	17
PROJECT- EXCAVATION	17
TOTAL 0010	34

FIELD OFFICE

CATEGORY	PROJECT	642.5201 FIELD OFFICE TYPE C EACH
0010	3840-06-71	1
TOTAL 0010		1

TRAFFIC CONTROL

CATEGORY	STAGE	STAGE DURATION DAYS	LOCATION	643.0300	643.0420	643.0900	643.0920	643.1000	643.1050	644.1410	644.1601	644.1605	644.1810
				TRAFFIC CONTROL DRUMS EACH**	TRAFFIC CONTROL BARRICADES TYPE III EACH**	TRAFFIC CONTROL SIGNS EACH**	TRAFFIC CONTROL COVERING SIGNS TYPE II EACH	TRAFFIC CONTROL SIGNS FIXED MESSAGE EACH**	TRAFFIC CONTROL SIGNS PCMS DAY	TEMPORARY PEDESTRIAN SURFACE ASPHALT SF	TEMPORARY PEDESTRIAN CURB RAMP EACH**	TEMPORARY PEDESTRIAN DETECTABLE WARNING FIELD EACH**	TEMPORARY PEDESTRIAN BARRICADE LF
0010	DETOUR	75	DETOUR RT	--	18	105	--	29	87	--	--	--	--
0010	1	11	W MARKET ST	72	4	8	--	--	--	271	6	6	500
0010	2	11	W MARKET ST	60	12	12	--	--	--	640	5	5	200
0010	3	11	W MARKET ST	57	14	15	--	--	--	532	6	6	250
UNDISTRIBUTED				520	338	1969	4	--	50	361	47	--	238
TOTAL 0010				2,599	2,018	10,229	4	87	50	1,804	234	204	1,188

**FOR INFORMATION ONLY

MOVING/REMOVING SIGNS

CATEGORY	SIGN NO	SIGN CODE	SIGN MESSAGE	638.2102	638.2602	638.4000	NOTES
				MOVING SIGNS TYPE II EACH	REMOVING SIGNS TYPE II EACH	MOVING SMALL SIGN SUPPORTS EACH	
0010	R200	R2-1	SPEED LIMIT 30	--	1	--	
0010	M303		N SANDY CT	1	--	1	WITH M304
0010	M304	R1-1	STOP	1	--	--	WITH M303
0010	R401	R2-1	SPEED LIMIT 30	--	1	--	
0010	R402	R2-1	SPEED LIMIT 30	--	1	--	
0010	M405		N PATRICIA CT	1	--	1	WITH M406
0010	M406	R1-1	STOP	1	--	1	WITH M405
0010	M503		HEATHER DR	1	--	1	WITH M504 & M505
0010	M504		E MARKET ST	1	--	1	WITH M503 & M505
0010	M505	R1-1	STOP	1	--	--	WITH M503 & M504
0010	R601	R2-1	SPEED LIMIT 30	--	1	--	
0010	R602	R2-1	SPEED LIMIT 30	--	1	--	
0010	R901	R2-1	SPEED LIMIT 30	--	1	--	
			TOTAL 0010	7	6	5	

TRAFFIC CONTROL

CATEGORY	LOCATION	643.5000
		TRAFFIC CONTROL EACH
0010	PROJECT 3840-06-71	1
	TOTAL 0010	1

GEOGRID

CATEGORY	LOCATION	645.0220
		GEOGRID TYPE SR SY
0010	MARKET ST EBS*	12,266
	TOTAL 0010	12,266

*TO BE USED WITH BASE AGGGREGATE DENSE 1 1/4-INCH AT EBS LOCATIONS DETERMINED BY THE ENGINEER.
SEE CONSTRUCTION DETAIL.

SIGNING

CATEGORY	SIGN NO	SIGN CODE	SIGN MESSAGE	SIGN SIZE	634.0814	637.2210	REMARKS
				W x H [IN.] x [IN.]	POSTS TUBULAR STEEL 2X2-INCH X 14-FT EACH	SIGNS TYPE II REFLECTIVE H SF	
0010	P100	R7-9	NO PARKING BIKE LANE	18.00 x 24.00	1	3.0	
0010	P101	R3-17	BIKE LANE	30.00 x 24.00	1	5.0	WITH P102
0010	P102	R3-17BP	ENDS	30.00 x 12.00	--	2.5	WITH P101
0010	P103	R7-9	NO PARKING BIKE LANE	18.00 x 24.00	1	3.0	
0010	P200	R2-1	SPEED LIMIT 30	24.00 x 30.00	--	5.0	
0010	P201	R7-9	NO PARKING BIKE LANE	18.00 x 24.00	1	3.0	
0010	P202	R7-9	NO PARKING BIKE LANE	18.00 x 24.00	1	3.0	
0010	P203	R3-17A	LEFT BIKE/RIGHT PARK ONLY	18.00 x 24.00	1	3.0	
0010	P300	R3-17A	LEFT BIKE/RIGHT PARK ONLY	18.00 x 24.00	1	3.0	
0010	P301	R7-9	NO PARKING BIKE LANE	18.00 x 24.00	1	3.0	
0010	P302	R3-17A	LEFT BIKE/RIGHT PARK ONLY	18.00 x 24.00	1	3.0	
0010	P401	R2-1	SPEED LIMIT 30	24.00 x 30.00	--	5.0	
0010	P402	R2-1	SPEED LIMIT 30	24.00 x 30.00	--	5.0	
0010	P403	R7-9	NO PARKING BIKE LANE	18.00 x 24.00	1	3.0	
0010	P404	R7-9	NO PARKING BIKE LANE	18.00 x 24.00	1	3.0	
0010	P407	R3-17A	LEFT BIKE/RIGHT PARK ONLY	18.00 x 24.00	1	3.0	
0010	P500	R7-9	NO PARKING BIKE LANE	18.00 x 24.00	1	3.0	
0010	P501	R7-9	NO PARKING BIKE LANE	18.00 x 24.00	1	3.0	
0010	P502	R3-17A	LEFT BIKE/RIGHT PARK ONLY	18.00 x 24.00	1	3.0	
0010	P600	R7-9	NO PARKING BIKE LANE	18.00 x 24.00	1	3.0	
0010	P601	R2-1	SPEED LIMIT 30	24.00 x 30.00	--	5.0	
0010	P602	R2-1	SPEED LIMIT 30	24.00 x 30.00	--	5.0	
0010	P603	R7-9	NO PARKING BIKE LANE	18.00 x 24.00	1	3.0	
0010	P604	R3-17A	LEFT BIKE/RIGHT PARK ONLY	18.00 x 24.00	1	3.0	
0010	P700	R7-9	NO PARKING BIKE LANE	18.00 x 24.00	1	3.0	
0010	P701	R7-9	NO PARKING BIKE LANE	18.00 x 24.00	1	3.0	
0010	P702	R3-17A	LEFT BIKE/RIGHT PARK ONLY	18.00 x 24.00	1	3.0	
0010	P800	R7-9	NO PARKING BIKE LANE	18.00 x 24.00	1	3.0	
0010	P801	R7-9	NO PARKING BIKE LANE	18.00 x 24.00	1	3.0	
0010	P802	R7-9	NO PARKING BIKE LANE	18.00 x 24.00	1	3.0	
0010	P803	R3-17A	LEFT BIKE/RIGHT PARK ONLY	18.00 x 24.00	1	3.0	
0010	P804	R3-17A	LEFT BIKE/RIGHT PARK ONLY	18.00 x 24.00	1	3.0	
0010	P900	R7-9	NO PARKING BIKE LANE	18.00 x 24.00	1	3.0	
0010	P901	R2-1	SPEED LIMIT 30	24.00 x 30.00	--	5.0	
0010	P902	R3-17	BIKE LANE	30.00 x 24.00	1	5.0	WITH P903
0010	P903	R3-17BP	ENDS	30.00 x 12.00	--	2.5	WITH P902
0010	P904	R7-9	NO PARKING BIKE LANE	18.00 x 24.00	1	3.0	
0010	P905	R3-17A	LEFT BIKE/RIGHT PARK ONLY	18.00 x 24.00	1	3.0	
			TOTAL 0010		30	129.0	

3

3

PAVEMENT MARKING

CATEGORY	ROADWAY	STATION	TO	STATION	646.1020 MARKING LINE EPOXY 4-INCH (WHITE)			646.5020	646.5220	646.6120	646.7420
					(YELLOW) LF	(WHITE) LF	(WHITE) (3 FT LINE 9 FT SKIP) LF	MARKING ARROW EPOXY EACH	MARKING SYMBOL EPOXY EACH	MARKING STOP LINE EPOXY 18- INCH LF	MARKING CROSSWALK EPOXY TRANSVERSE LINE 6- INCH LF
0010	MARKET	203+50	-	208+00	1,338	1,351	--	3	3	--	--
0010	MARKET	208+00	-	214+00	1,102	1,735	--	4	4	--	136
0010	MARKET	214+00	-	220+00	1,082	1,262	--	6	6	--	393
0010	MARKET	220+00	-	226+00	1,000	1,486	--	4	4	--	318
0010	MARKET	226+00	-	232+00	996	1,332	70	5	5	--	437
0010	MARKET	232+00	-	238+00	1,198	1,804	--	4	4	--	--
0010	MARKET	238+00	-	244+00	1,200	1,801	--	5	5	--	--
0010	MARKET	244+00	-	250+00	1,196	1,806	--	5	5	--	--
0010	MARKET	250+00	-	254+15	836	1,236	--	3	3	17	--
SUB TOTAL					9,948	13,813	70	39	39	17	1,284
TOTAL 0010					23,831			39	39	17	1,284

SAWING

CATEGORY	LOCATION	690.0150	690.0250
		SAWING ASPHALT LF	SAWING CONCRETE LF
0010	MARKET ST BEGIN	40	--
0010	SWEETBRIAR DR	134	41
0010	MICHAEL CT NORTH	121	18
0010	MICHAEL CT SOUTH	113	22
0010	SANDY CT	112	27
0010	SANDY LN	140	25
0010	CHELSEA DR	136	26
0010	PATRICIA CT	140	21
0010	PATRICIA ST	128	26
0010	KELLY LN	117	24
0010	SUSAN CT WEST	91	12
0010	SUSAN CT EAST	112	24
0010	HEATHER DR	143	26
0010	MARKET ST END	40	--
0010	DRIVEWAYS	128	40
0010	MARKET ST INLET/MANHOLE REPAIRS	140	20
TOTAL 0010		1,835	352

TEMPORARY PAVEMENT MARKING

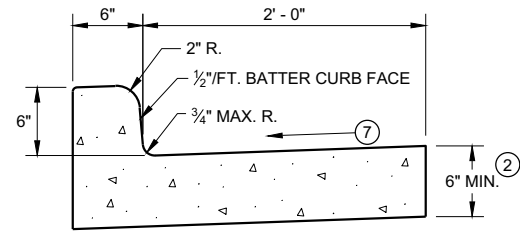
CATEGORY	STAGE	STATION	TO	STATION	643.3105
					TEMPORARY MARKING LINE PAINT 4-INCH LF
0010	1	214+00	-	220+00	174
0010	1	220+00	-	226+00	110
0010	1	226+00	-	232+00	82
STAGE 1 SUBTOTAL					366
0010	2	208+00	-	214+00	61
0010	2	214+00	-	220+00	142
0010	2	220+00	-	226+00	185
0010	2	226+00	-	232+00	175
STAGE 2 SUBTOTAL					563
0010	3	208+00	-	214+00	142
0010	3	214+00	-	220+00	94
0010	3	220+00	-	226+00	60
0010	3	226+00	-	232+00	154
STAGE 3 SUBTOTAL					450
TOTAL 0010					1,379

SPECIAL PROVISIONS

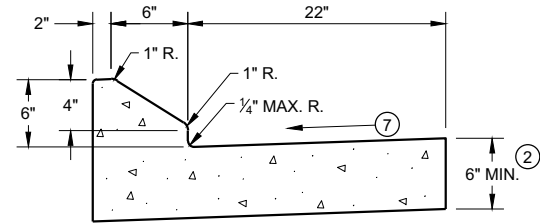
CATEGORY	SPV.0060.01	SPV.0060.02	SPV.0060.51	SPV.0060.52	SPV.0075.01
	SPECIAL (SURVEY PROJECT) EACH	SPECIAL (CURB RAMP GRADING, SHAPING, AND FINISHING) EACH	SPECIAL (ADJUSTING SANITARY MANHOLE) EACH	SPECIAL (ADJUSTING WATER VALVE BOXES) EACH	SPECIAL (PAVEMENT CLEANUP PROJECT) HRS
0010	1	30	5	5	16
TOTAL 0010	1	30	5	5	16

Standard Detail Drawing List

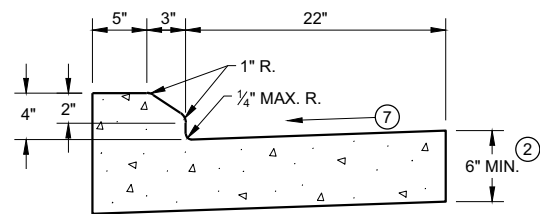
08D01-22A	CONCRETE CURB & GUTTER
08D01-22B	CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS
08D05-20A	CURB RAMPS TYPES 1 AND 1-A
08D05-20B	CURB RAMPS TYPES 2 AND 3
08D05-20C	CURB RAMPS TYPES 4A AND 4A1
08D05-20D	CURB RAMPS TYPE 4B AND 4B1
08D05-20E	CURB RAMPS TYPES 5, 6, 7A, 7B & 8
08D05-20F	CURB RAMPS RADIAL DETECTABLE WARNING FIELD APPLICATIONS
08D05-20G	CURB RAMPS RECTANGULAR AND RADIAL DETECTABLE WARNING PLATES
08D19-03	DRIVEWAY AND SIDEWALK RAMPS TYPE Z
08E10-02	INLET PROTECTION TYPE A, B, C AND D
13C19-03	HMA LONGITUDINAL JOINTS
15C02-08A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-08B	BARRICADES AND SIGNS FOR VARIOUS CLOSURES
15C02-08C	DETOUR SIGNING FOR MAINLINE CLOSURES
15C03-05	BARRICADES AND SIGNS FOR SIDEROAD CLOSURES
15C07-15E	PAVEMENT MARKING FOR BIKE LANES
15C08-21A	LONGITUDINAL MARKING (MAINLINE)
15C11-09B	CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS
15C29-07A	BICYCLE LANE MARKING
15C33-04	STOP LINE AND CROSSWALK PAVEMENT MARKING
15C35-05A	PAVEMENT MARKING (INTERSECTIONS)
15D30-07A	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-07B	TRAFFIC CONTROL, TEMPORARY ADA COMPLIANT PEDESTRIAN ACCOMMODATION
15D30-07C	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-07D	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-07E	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-07F	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-07G	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-07H	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-07I	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-07J	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D39-02	TRAFFIC CONTROL, DROP-OFF SIGNING
15D45-03	TRAFFIC CONTROL, SIGNING ON ROADWAYS WITH LOOSE GRAVEL



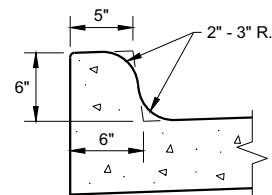
TYPES A¹ & D



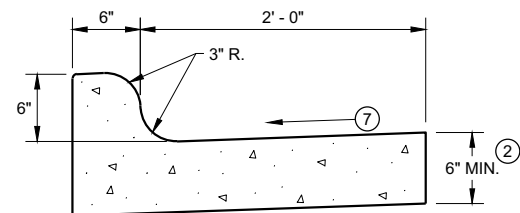
6" SLOPED CURB TYPES G¹ & J



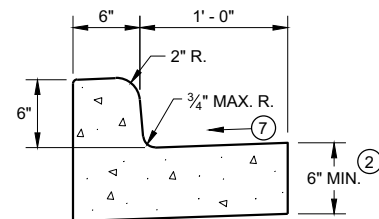
4" SLOPED CURB TYPES G¹ & J



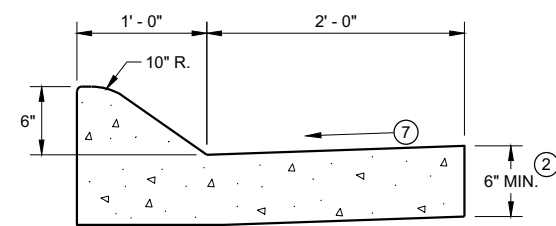
TYPES K¹ & L
(OPTIONAL CURB SHAPE)



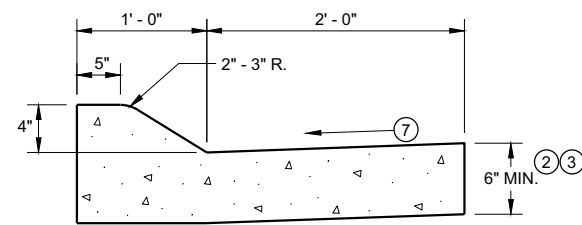
TYPES K¹ & L
CONCRETE CURB AND GUTTER 30"



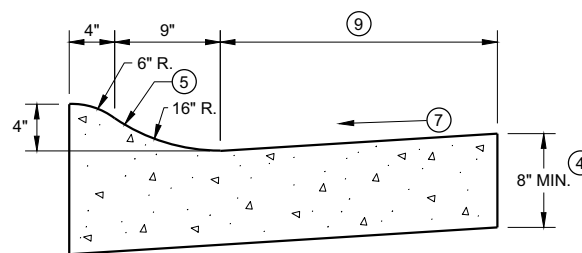
TYPES A¹ & D
CONCRETE CURB AND GUTTER 18"



6" SLOPED CURB TYPES A¹ & D

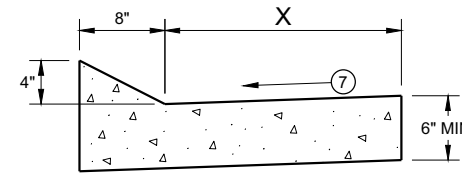


4" SLOPED CURB TYPES A¹ & D
CONCRETE CURB AND GUTTER 36"



4" SLOPED CURB TYPES R¹ & T

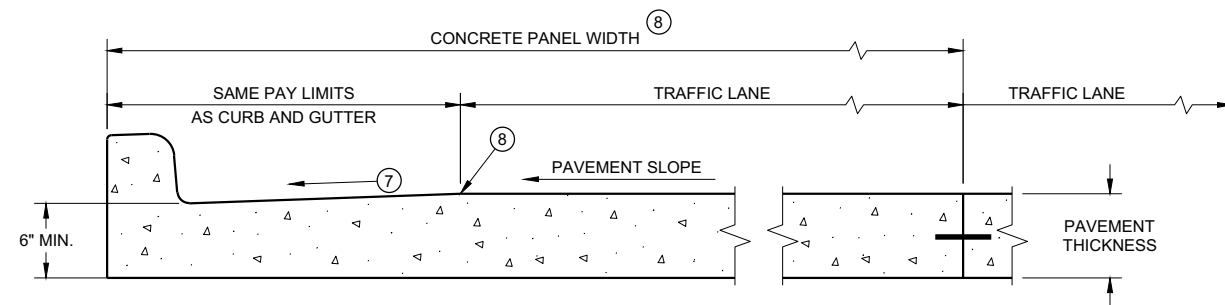
TBT & TBTT	X
30"	22"
36"	28"



TYPES TBT & TBTT¹
CONCRETE CURB AND GUTTER

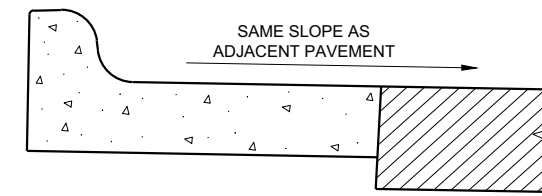
PAVEMENT THICKNESS AND MAXIMUM CONCRETE PANEL WIDTH TABLE

PAVEMENT THICKNESS	MAXIMUM PANEL WIDTH
LESS THAN 10"	12'
10" & ABOVE	15'



PARTIAL SECTION OF PAVEMENT *
WITH INTEGRAL CURB AND GUTTER

* BIKE LANE IS NOT SHOWN



REVERSE SLOPE GUTTER⁶
(TYPICAL FOR ALL CURB & GUTTER TYPES)

GENERAL NOTES

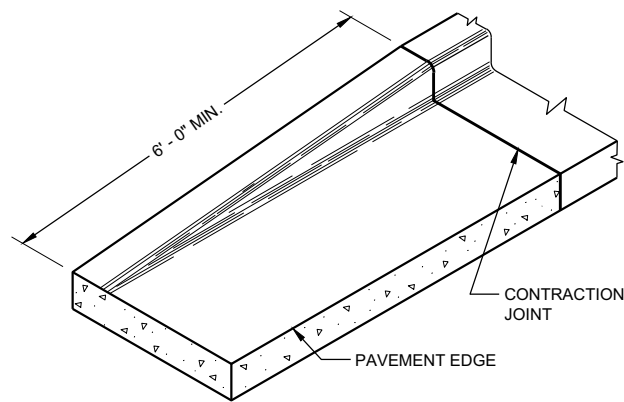
DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

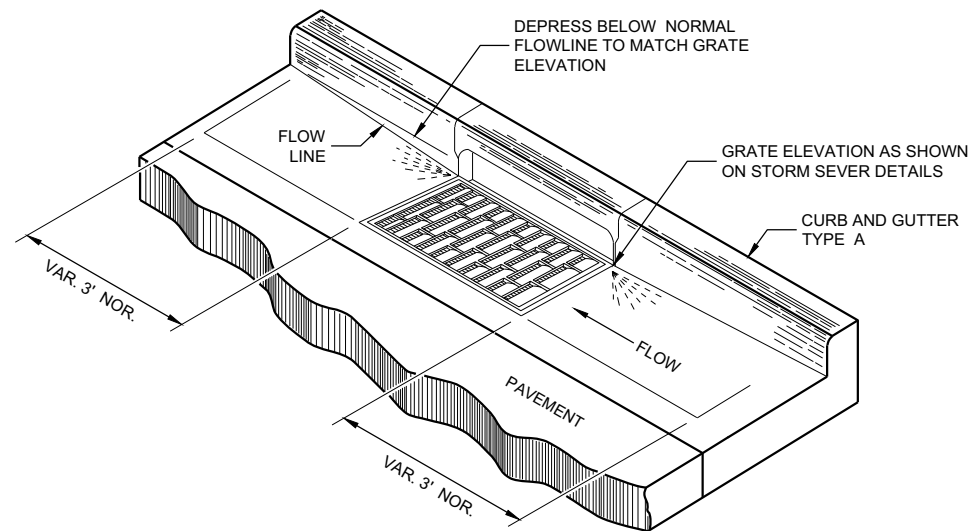
INTEGRAL CURB AND GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB AND GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

- 1 TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
- 2 THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- 3 USE 8" MINIMUM GUTTER THICKNESS WHEN USED WITH AN ADJACENT CONCRETE TRUCK APRON PLACED BEHIND BACK OF CURB.
- 4 THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- 5 UNLESS OTHERWISE NOTED, FOR STAKING PURPOSES THE FACE OF CURB IS 6" FROM THE BACK OF CURB.
- 6 WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.
- 7 USE 4% GUTTER CROSS SLOPE UNLESS OTHERWISE NOTED IN THE PLANS.
- 8 INCLUDE LONGITUDINAL JOINT AND TIE BARS ALONG LANE EDGE WHEN CONCRETE PANEL WIDTH EXCEEDS THE MAXIMUM WIDTH PER TABLE BELOW. LONGITUDINAL JOINT(S) ARE NOT ALLOWED WITHIN TRAFFIC LANES AND BIKE LANES. LONGITUDINAL JOINT MAY BE SAWED.
- 9 CONCRETE CURB AND GUTTER 4-INCH SLOPED 30-INCH TYPE "R" AND "T" = 17 INCHES
CONCRETE CURB AND GUTTER 4-INCH SLOPED 36-INCH TYPE "R" AND "T" = 23 INCHES



END SECTION CURB AND GUTTER



DETAIL OF CURB AND GUTTER AT INLETS
(TYPICAL H INLET COVER SHOWN)

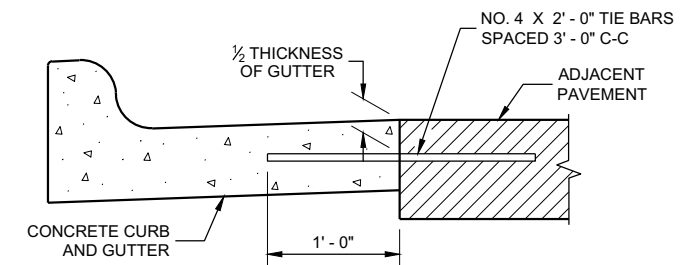
GENERAL NOTES

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

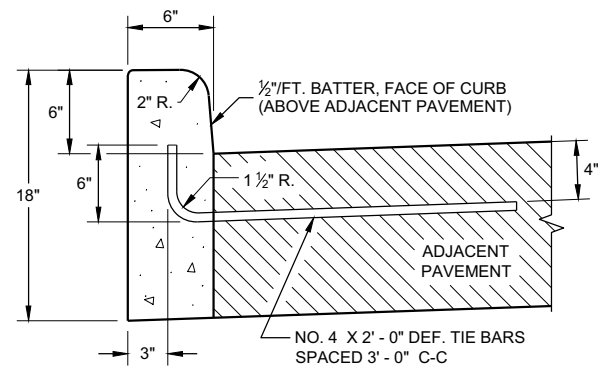
PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

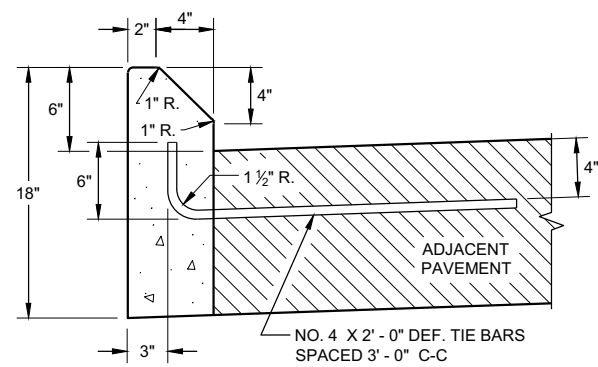
- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑨ REFER TO SDD 08D18 AND 08D19 FOR ADDITIONAL DRIVEWAY ENTRANCE CURB DETAILS.



TYPICAL TIE BAR LOCATION ①

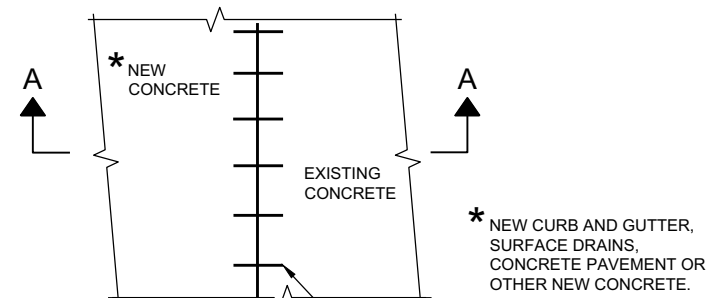


TYPES A ① & D

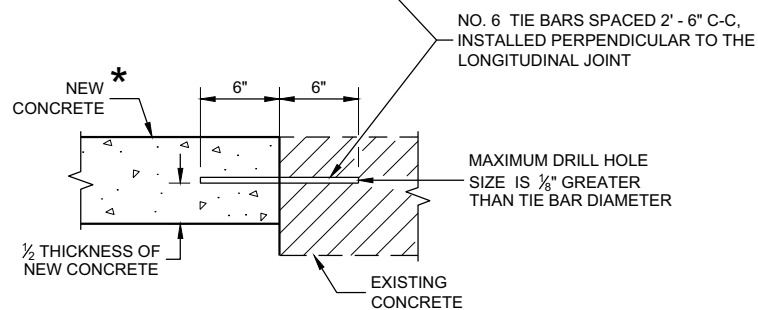


TYPES G ① & J

CONCRETE CURB

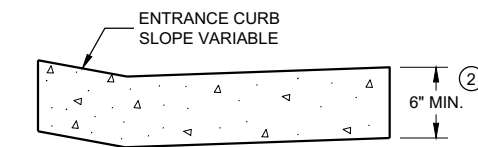


PLAN VIEW



SECTION A - A

TIE BARS DRILLED INTO EXISTING PAVEMENT



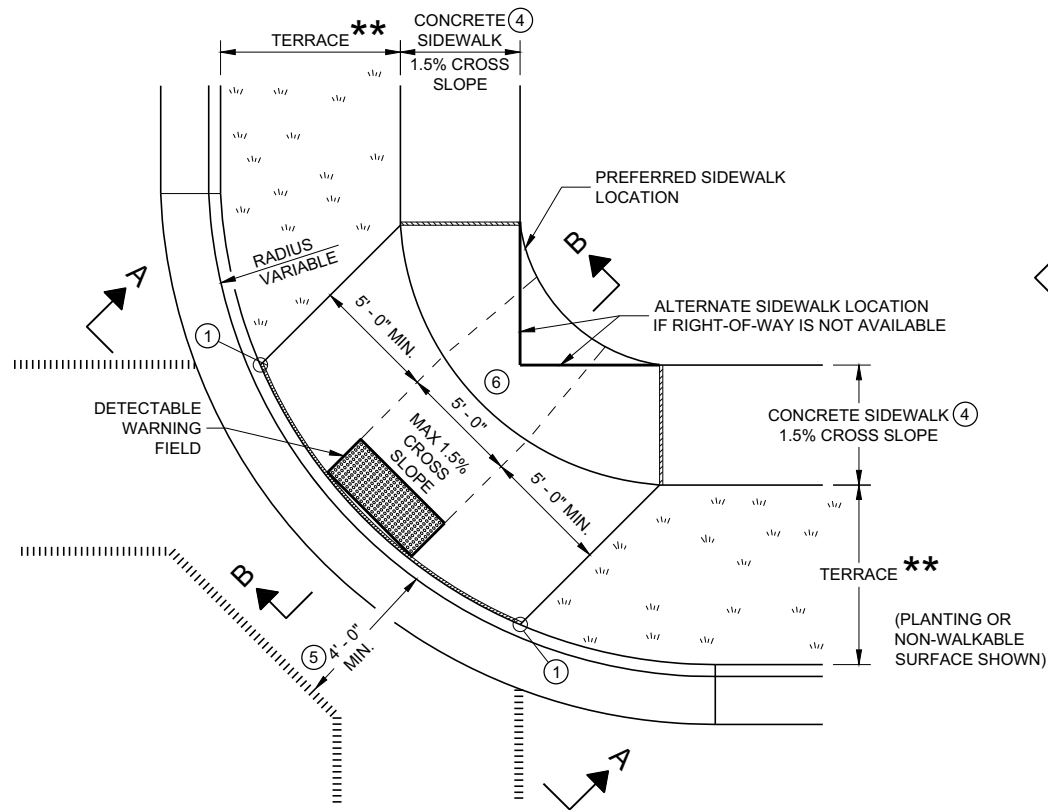
DRIVEWAY ENTRANCE CURB ⑨
(WHEN DIRECTED BY THE ENGINEER)

CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS

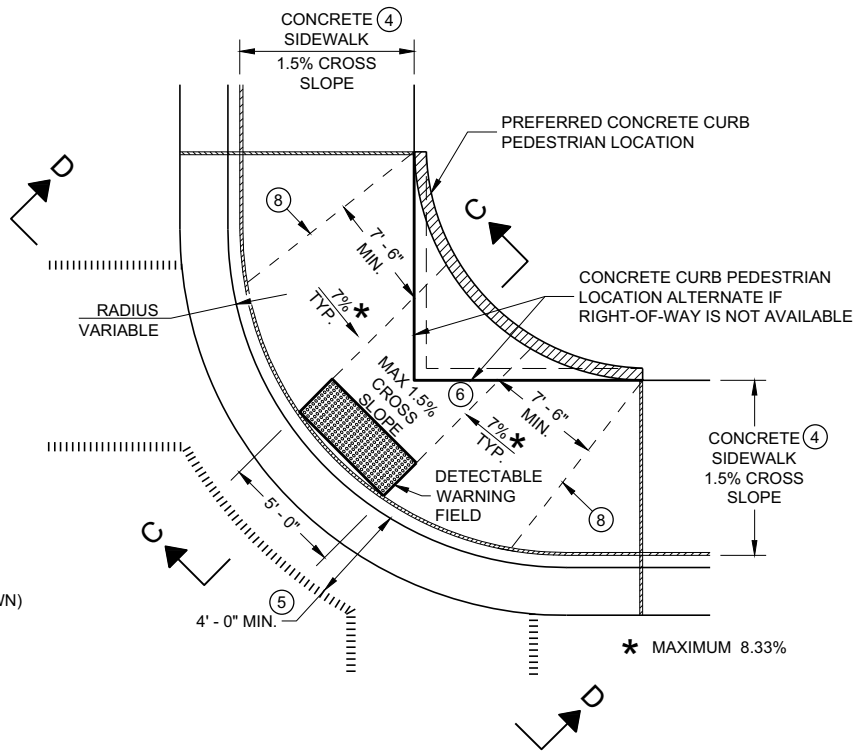
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2021 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
ENGINEER

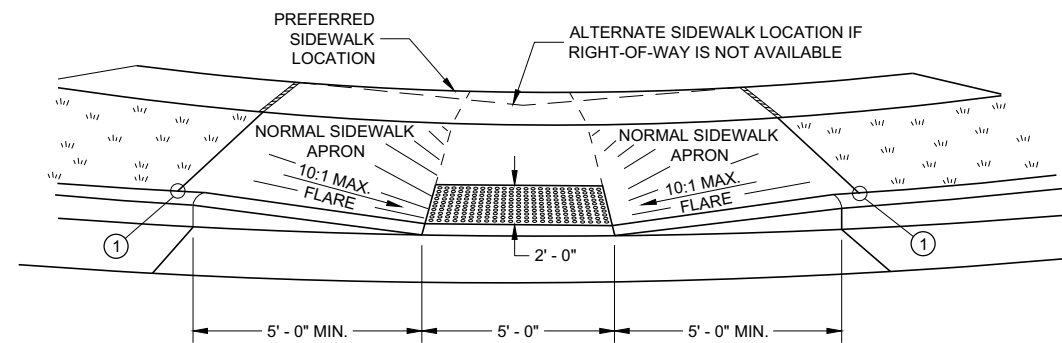
FHWA



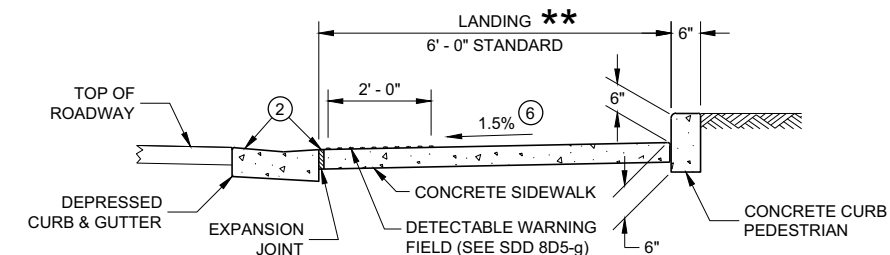
**PLAN VIEW
CURB RAMP TYPE 1
(CENTER OF CORNER RADIUS)**



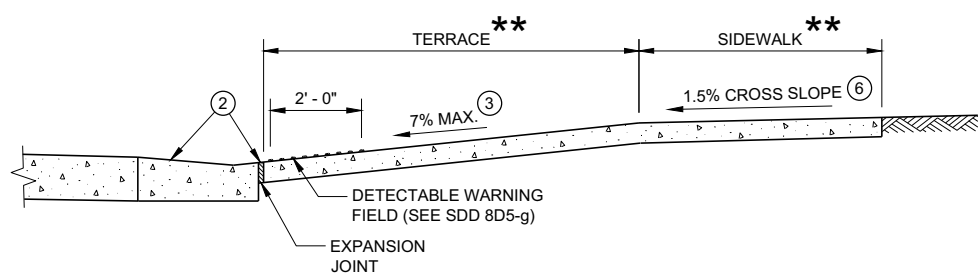
**PLAN VIEW
CURB RAMP TYPE 1 - A
(NO TERRACE)**



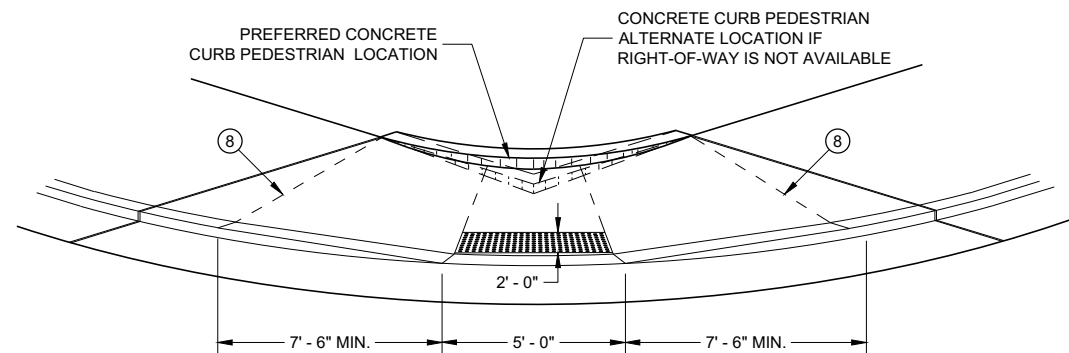
VIEW A - A FOR TYPE 1



SECTION C - C FOR TYPE 1 - A



SECTION B - B FOR TYPE 1



VIEW D - D FOR TYPE 1 - A

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
 DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

WHEN NECESSARY, THE SIDEWALK ELEVATION MAY BE LOWERED TO MEET THE HIGH POINT ON THE RAMP.
 TYPE 1 CURB RAMPS SHALL HAVE A NORMAL SIDEWALK APRON AND CURB ON BOTH SIDES OF RAMP.

DETECTABLE WARNING FIELD SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS "CURB RAMP DETECTABLE WARNING FIELD". THE CONCRETE PEDESTRIAN CURB, IF NEEDED, SHALL BE MEASURED AND PAID BY THE LINEAR FOOT AS "CONCRETE CURB PEDESTRIAN". CONCRETE SIDEWALK IN THE CURB RAMP AREA SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS CONCRETE SIDEWALK, INCLUDING THE AREA UNDER THE DETECTABLE WARNING FIELD.

SELECT CURB RAMP DETECTABLE WARNING FIELD MATERIALS AND DEVICES FROM THE DEPARTMENT'S APPROVED MATERIALS LIST. THE COLOR OF THE DETECTABLE WARNING FIELD IS SPECIFIED ELSEWHERE AND IS INCIDENTAL TO THE BID ITEM OF "CURB RAMP DETECTABLE WARNING FIELD"

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

SURFACE TEXTURE OF THE RAMP SHALL BE OBTAINED BY COARSE BROOMING TRANSVERSE TO THE SLOPE OF THE RAMP.

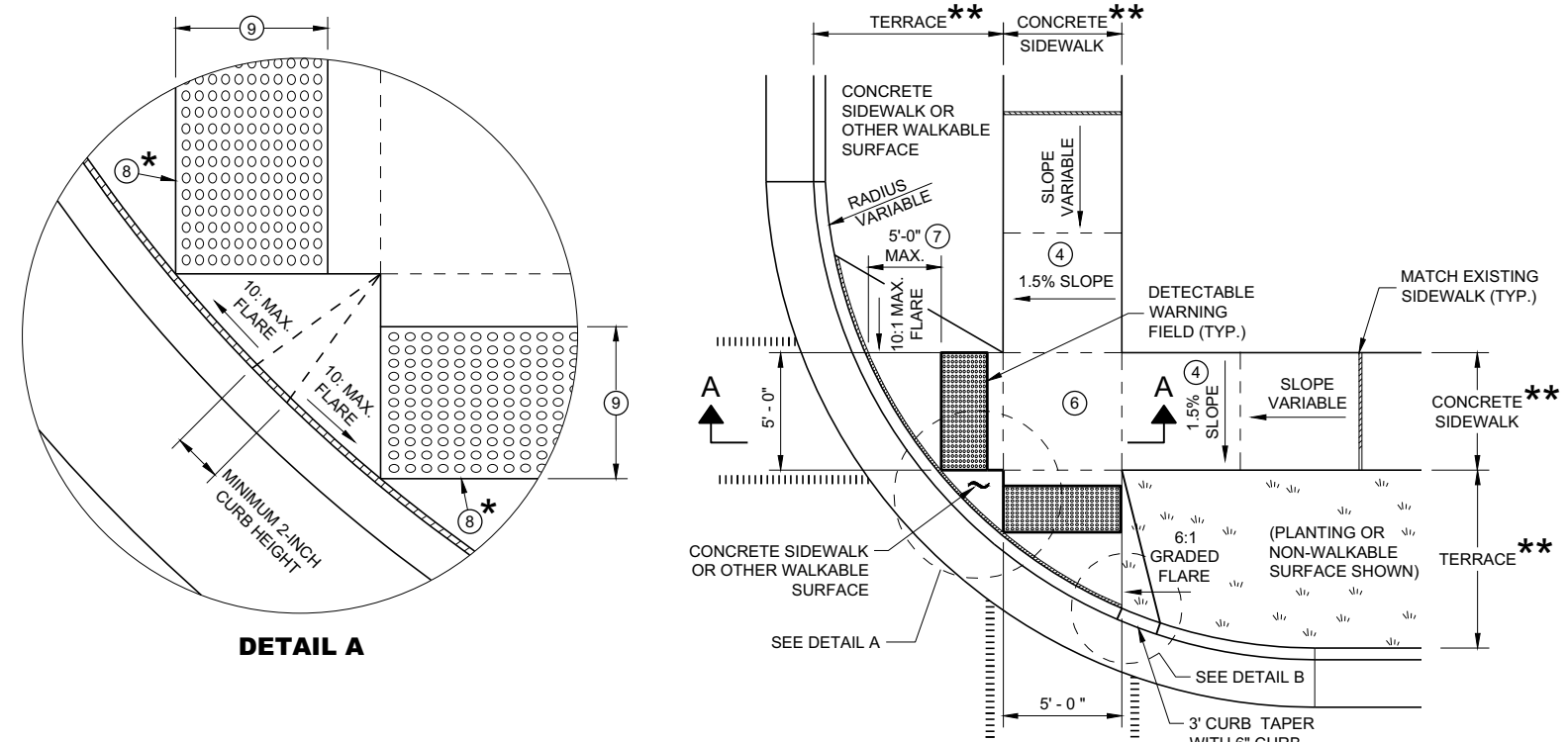
- ① THIS POINT IS AN EXTENSION OF OUTSIDE EDGE OF APPROACHING SIDEWALK WHERE IT MEETS THE BACK OF CONCRETE CURB. POINT LOCATION MAY BE ADJUSTED TO ALIGN WITH BEGINNING OF FULL-HEIGHT CURB IF THIS DISTANCE IS SHORT.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4" INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ MAXIMUM 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑤ PROVIDE A LEVEL LANDING IN THE STREET AND GUTTER AREA (2% MAXIMUM SLOPE IN ANY DIRECTION). WHEN THE GUTTER SLOPE EXCEEDS 2%, CONSTRUCT THE LEVEL LANDING IN THE STREET AREA.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

LEGEND

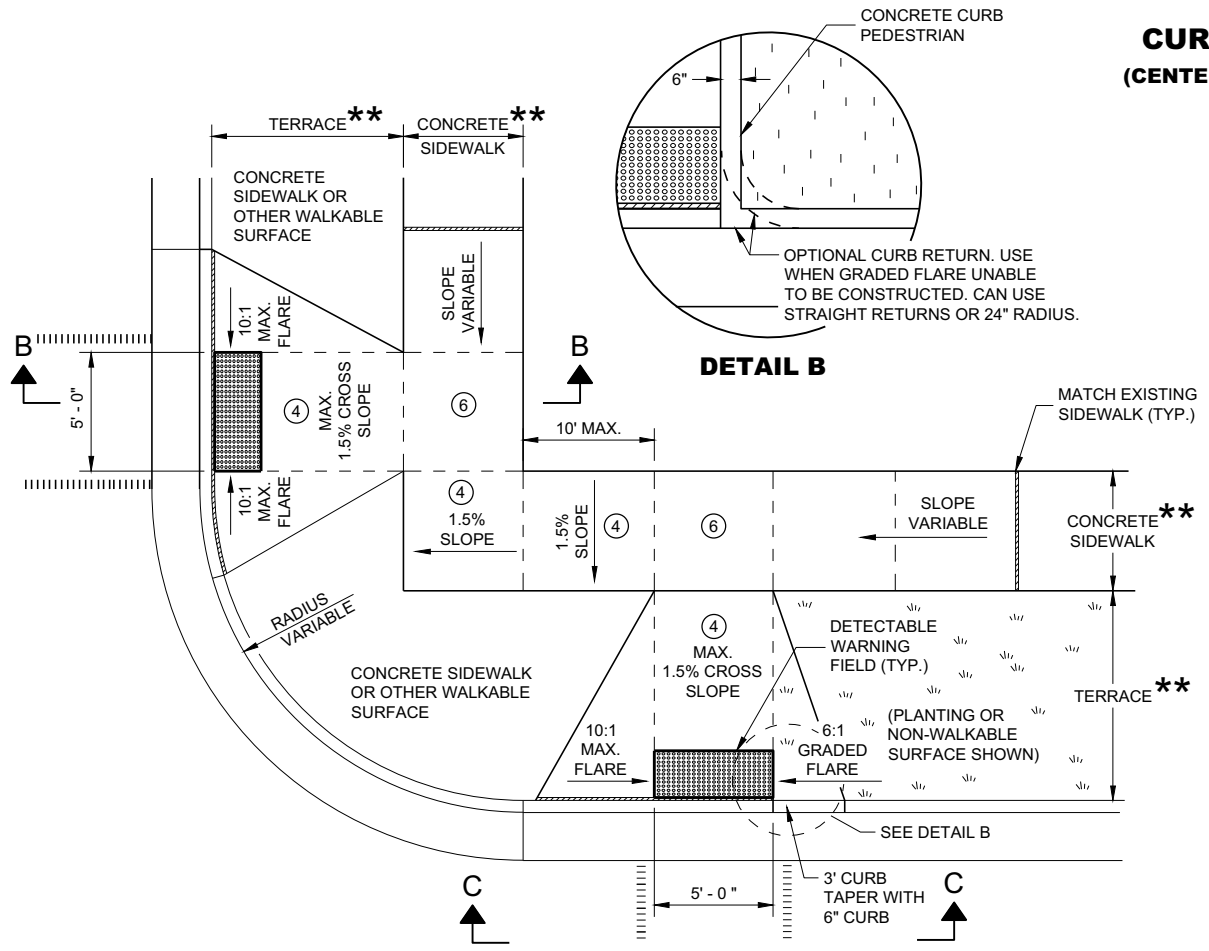
- 1/2" EXPANSION JOINT SIDEWALK
- - - - - CONTRACTION JOINT FIELD LOCATED
- ||||||| PAVEMENT MARKING CROSSWALK (WHITE)

**CURB RAMPS
TYPE 1 AND 1-A**

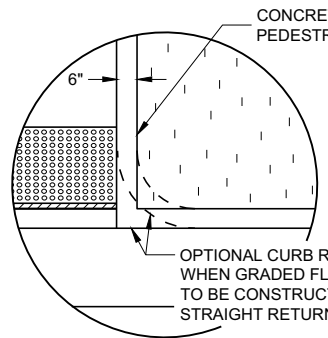
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



PLAN VIEW CURB RAMP TYPE 2 (CENTER OF CORNER RADIUS)



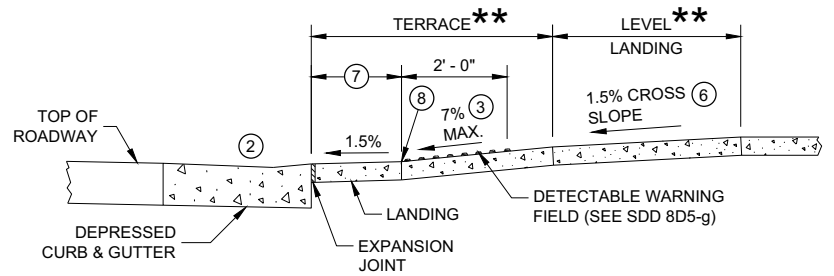
PLAN VIEW CURB RAMP TYPE 3 (OUTSIDE OF CROSSWALK AREA)



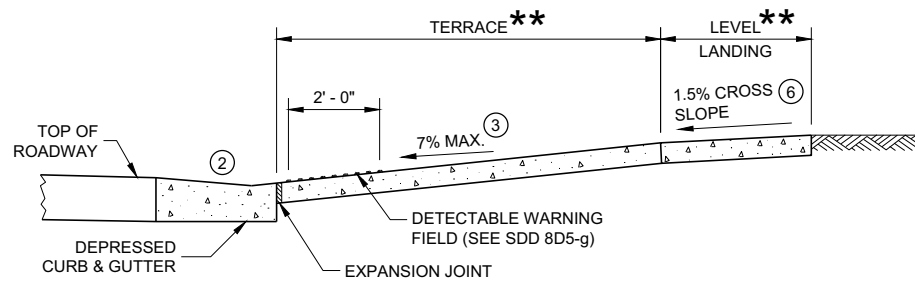
DETAIL B

GENERAL NOTES

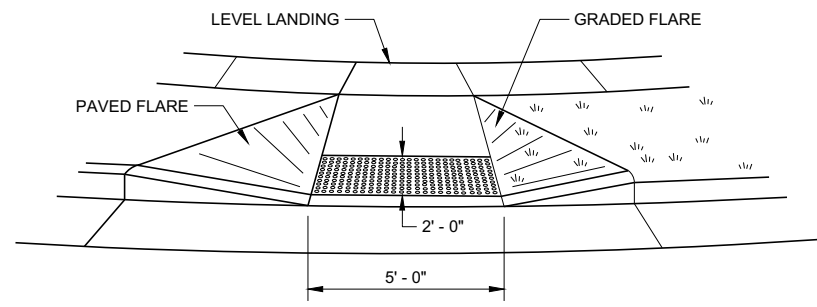
- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE (2.67% OR LESS) AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET X 5 FEET.
- ⑦ WHEN GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑨ WHEN DISTANCE IS LESS THAN 6' - 0", IT MAY BE DIFFICULT TO ACHIEVE A 7% SLOPE OR FLATTER ALONG THE RAMP. REDUCE CURB HEIGHT IN TRIANGLE AREA TO ACHIEVE 7% SLOPE OR FLATTER ON RAMP. CONSTRUCT 2-INCH MINIMUM CURB HEIGHT BETWEEN 10:1 FLARES.



SECTION A - A FOR TYPE 2



SECTION B - B FOR TYPE 3



VIEW C - C FOR TYPE 3

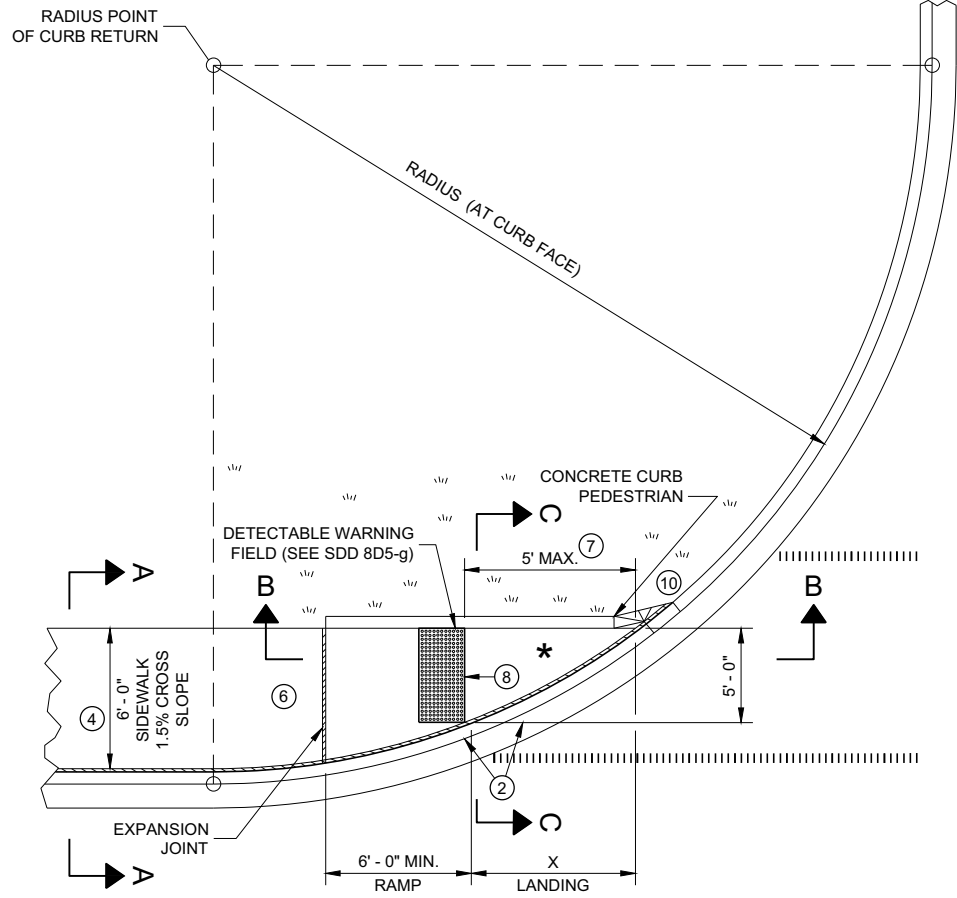
- * MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK
- ** WIDTH SHOWN ELSEWHERE IN THE PLANS

LEGEND

- 1/2" EXPANSION JOINT SIDEWALK
- - - - - CONTRACTION JOINT SIDEWALK
- ||||| PAVEMENT MARKING CROSSWALK (WHITE)

CURB RAMPS TYPE 2 AND 3

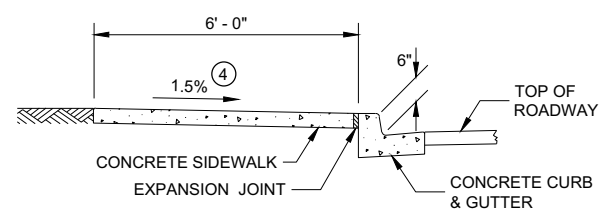
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**PLAN VIEW
CURB RAMP TYPE 4A**

RADIUS (AT CURB FACE)	X
10 FEET	4' - 7"
15 FEET	6' - 5 1/2"

INTERMEDIATE RADII CAN BE INTERPOLATED



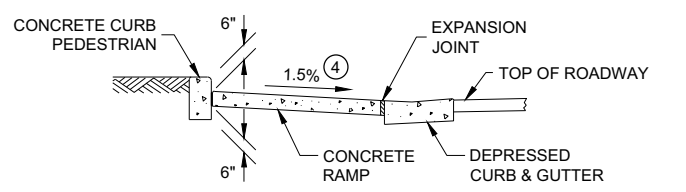
SECTION A - A FOR TYPE 4A

GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4" INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑦ WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑩ INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

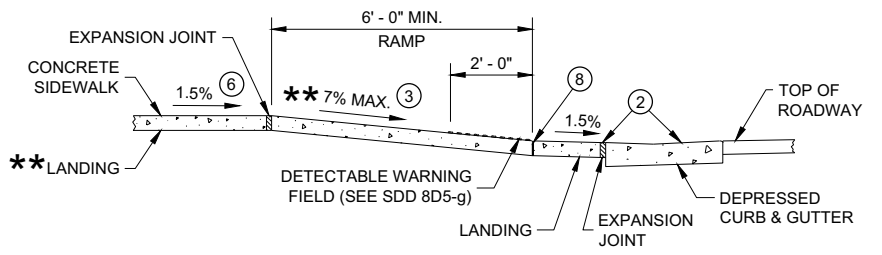
LEGEND

- 1/2" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT SIDEWALK
- PAVEMENT MARKING CROSSWALK (WHITE)



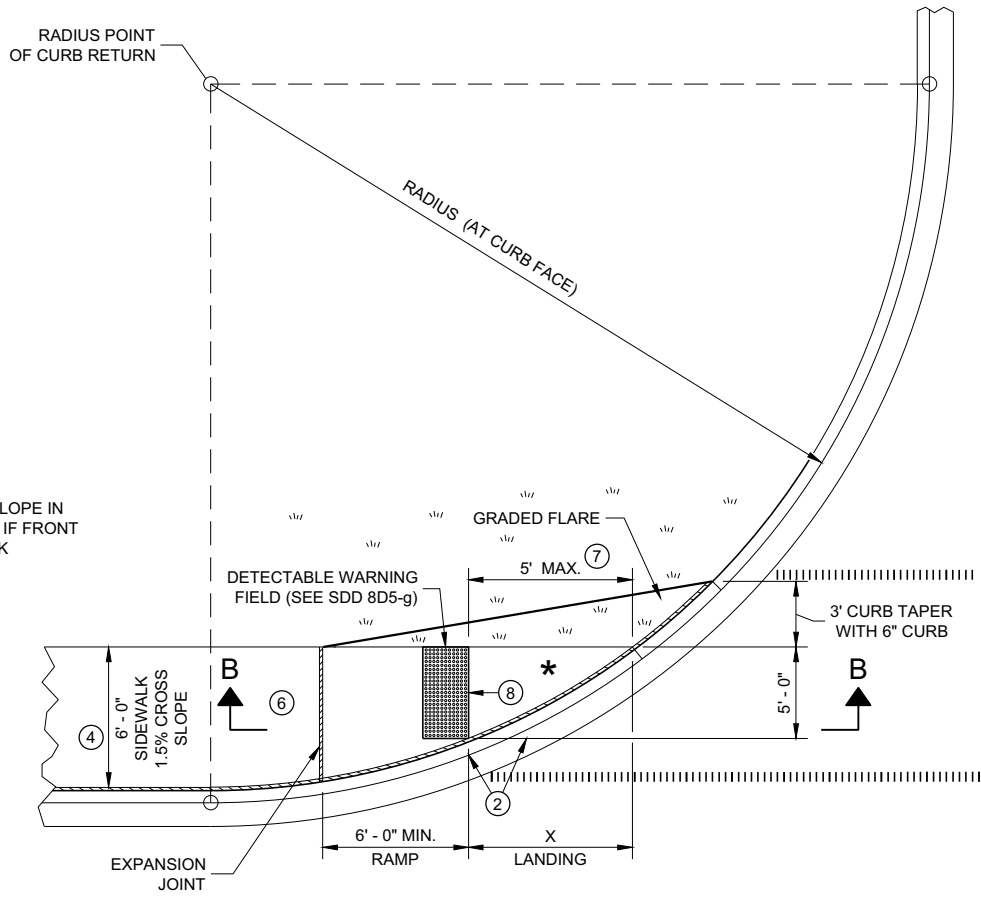
SECTION C - C FOR TYPE 4A

* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IF FRONT OF GRADE BREAK

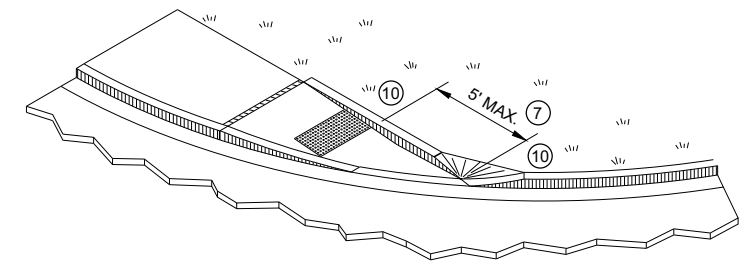


**SECTION B - B FOR
TYPE 4A AND TYPE 4A1**

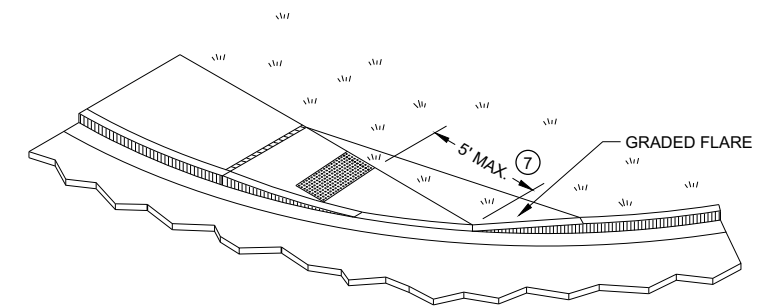
** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED



**PLAN VIEW
CURB RAMP TYPE 4A1**



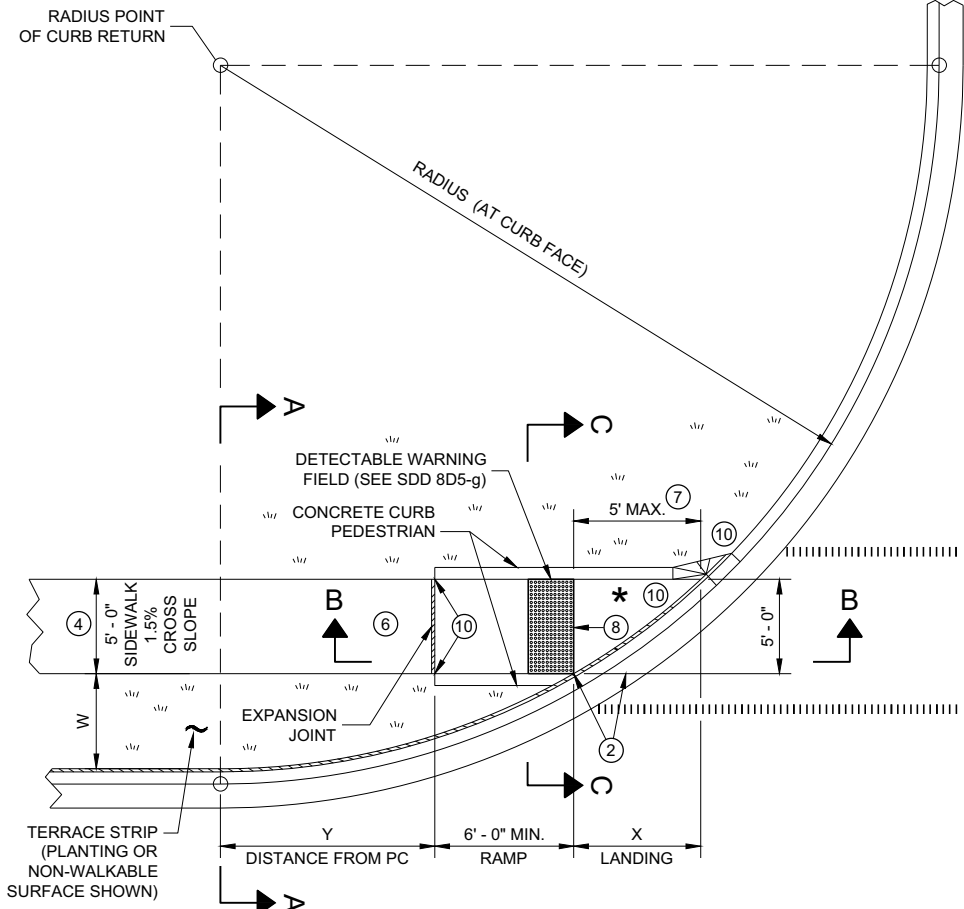
ISOMETRIC VIEW FOR TYPE 4A



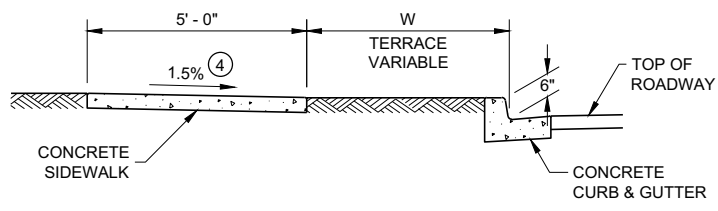
ISOMETRIC VIEW FOR TYPE 4A1

**CURB RAMPS
TYPE 4A AND 4A1**

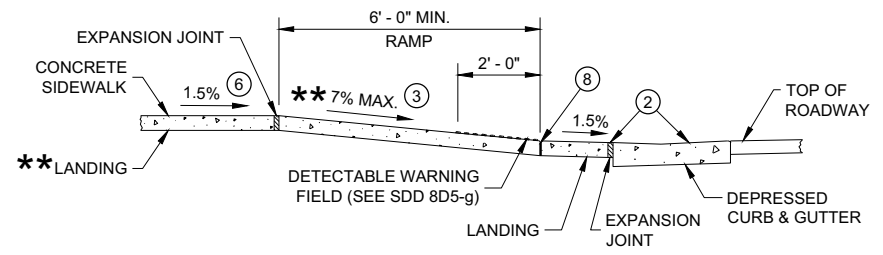
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**PLAN VIEW
CURB RAMP TYPE 4B**



SECTION A - A FOR TYPE 4B

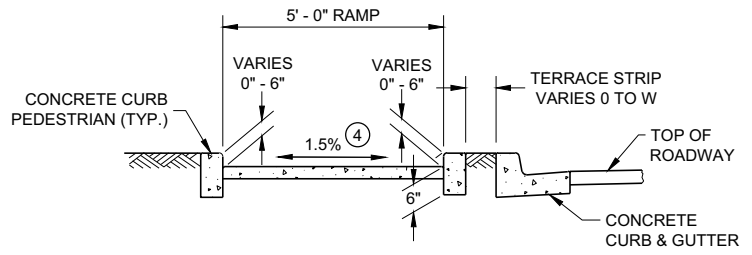


**SECTION B - B FOR
TYPE 4B AND TYPE 4B1**

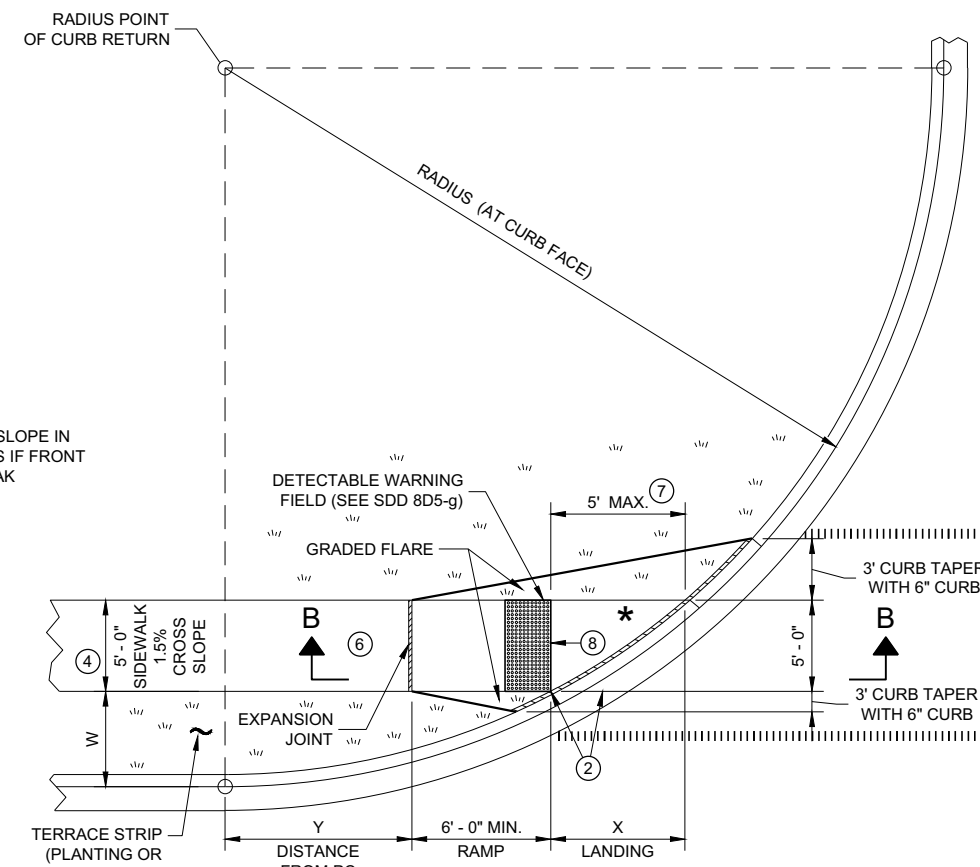
** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

RADIUS (AT CURB FACE)	W = 3' - 0"		W = 4' - 0"		W = 5' - 0"		W = 6' - 0"		W = 7' - 0"		W = 8' - 0"		W = 9' - 0"		W = 10' - 0"	
	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y
10 FEET	2' - 10 1/4"	0' - 5"	2' - 1"	1' - 4 1/2"	1' - 5"	2' - 1"	0' - 10"	2' - 7 1/2"	0' - 3 1/4"	3' - 0 1/4"						
15 FEET	4' - 6 3/4"	2' - 1 3/4"	3' - 9"	3' - 5 3/4"	3' - 1 1/4"	4' - 6"	2' - 6 3/4"	5' - 4 1/2"	2' - 1"	6' - 1"	1' - 8"	6' - 8 1/2"	1' - 3 1/4"	7' - 2 1/2"	0' - 10 3/4"	7' - 7 1/4"
20 FEET	5' - 9 3/4"	3' - 6 1/2"	4' - 11 1/2"	5' - 1 3/4"	4' - 3 1/4"	6' - 5 1/2"	3' - 8 3/4"	7' - 7"	3' - 3"	8' - 6 1/2"	2' - 10"	9' - 4 1/2"	2' - 5 1/2"	10' - 1 1/4"	2' - 1 1/4"	10' - 9"
30 FEET			6' - 9 1/4"	7' - 11 1/4"	6' - 0 1/4"	9' - 8"	5' - 5"	11' - 1 3/4"	4' - 10 3/4"	12' - 5 3/4"	4' - 5 1/2"	13' - 7 3/4"	4' - 0 3/4"	14' - 8 1/2"	3' - 8 1/2"	15' - 8 1/4"
40 FEET									6' - 1 3/4"	15' - 8 1/2"	5' - 8"	17' - 2"	5' - 3"	18' - 5 3/4"	4' - 10 3/4"	19' - 8 1/4"
50 FEET															5' - 10 1/4"	23' - 2"

INTERMEDIATE RADII CAN BE INTERPOLATED
DIMENSION "Y" IS CALCULATED BASED ON 6'-0" RAMP LENGTH
DIMENSION "X" IS CALCULATED BASED ON 5'-0" SIDEWALK WIDTH



SECTION C - C FOR TYPE 4B



**PLAN VIEW
CURB RAMP TYPE 4B1**

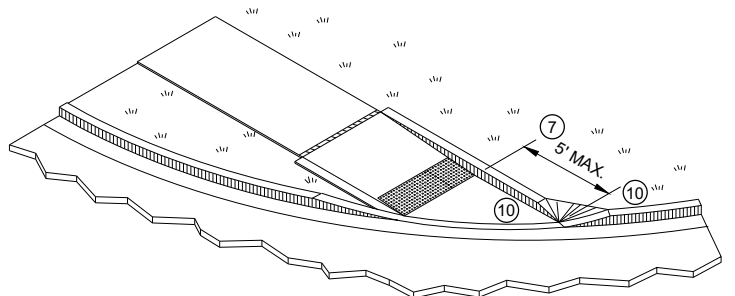
* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IF FRONT OF GRADE BREAK

LEGEND

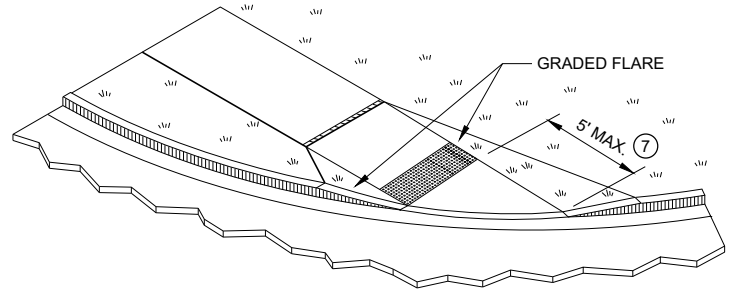
- 1/2" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT SIDEWALK
- PAVEMENT MARKING CROSSWALK (WHITE)

GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/2 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.



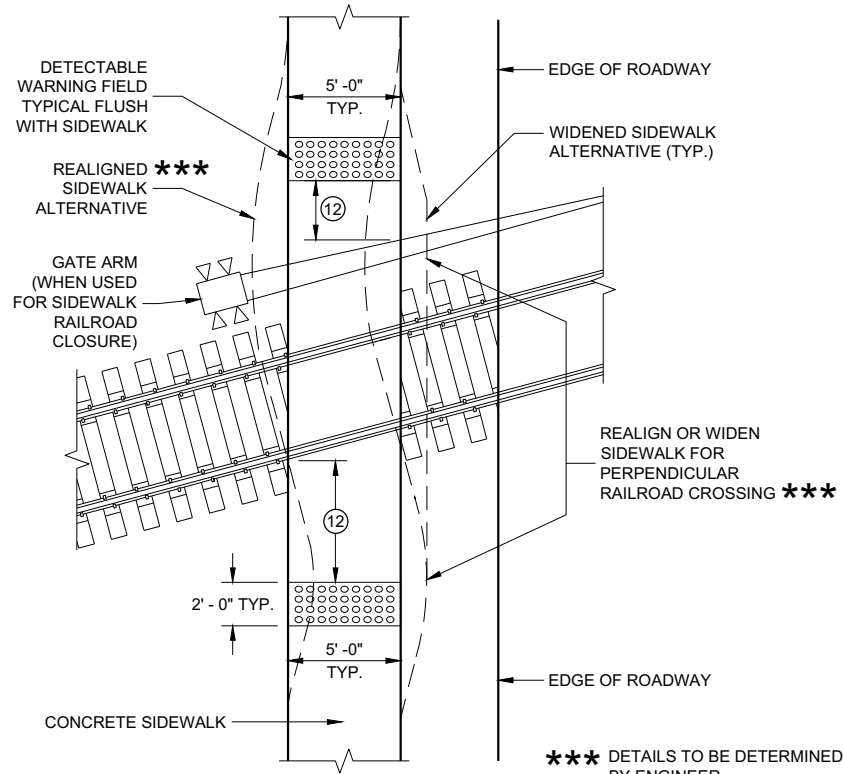
ISOMETRIC VIEW FOR TYPE 4B



ISOMETRIC VIEW FOR TYPE 4B1

**CURB RAMPS
TYPE 4B AND 4B1**

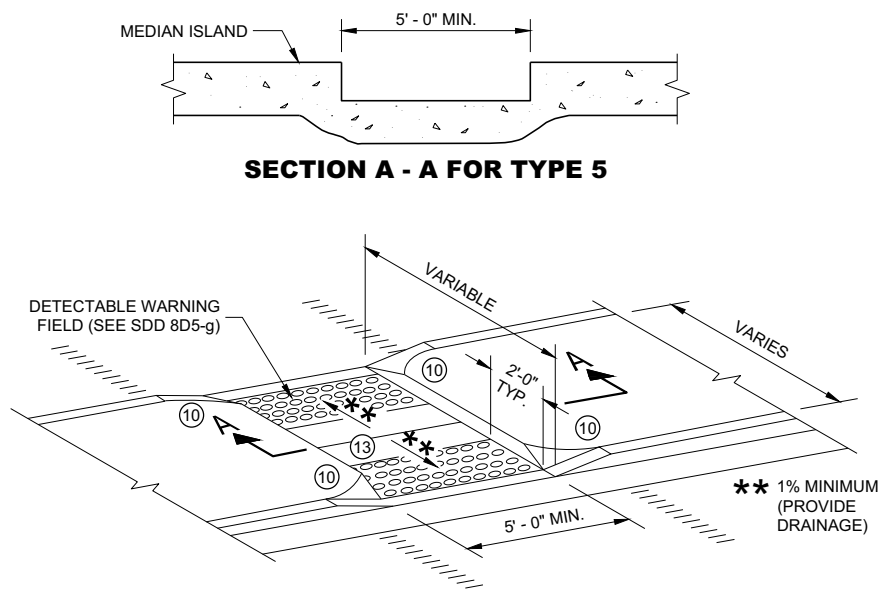
STATE OF WISCONSIN
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CURB RAMP TYPE 8

DETECTABLE WARNINGS AT RAILROAD CROSSING

*** DETAILS TO BE DETERMINED BY ENGINEER



SECTION A - A FOR TYPE 5

CURB RAMP TYPE 5

**MEDIAN ISLAND
NON-ELEVATED PEDESTRIAN CROSSING**

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

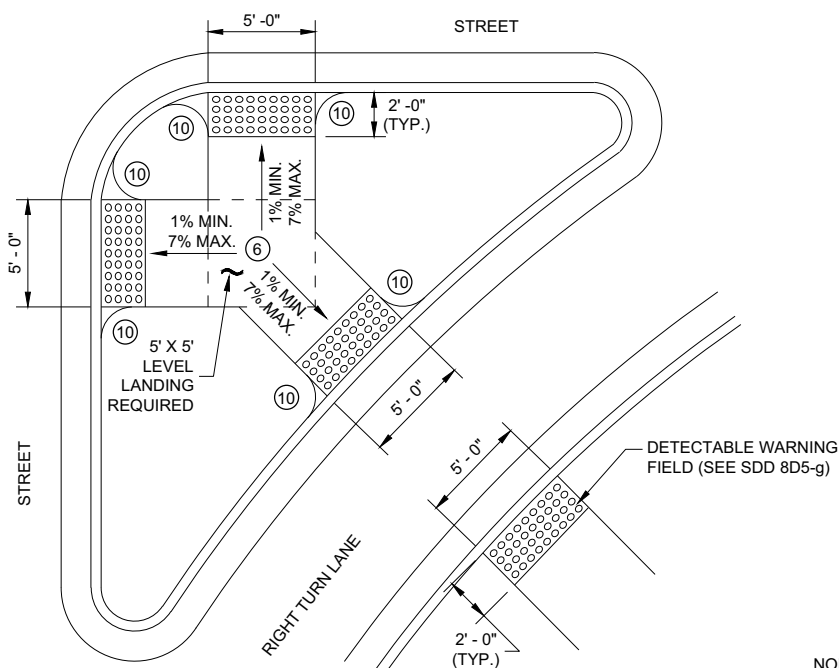
SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑩ INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.
- ⑪ SLOPE SIDEWALK TOWARD LANDING AS SHOWN WHERE THERE IS NO TERRACE OR WHERE THE TERRACE WIDTH IS LESS THAN 6 FEET WIDE.
- ⑫ THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO A RAILROAD CROSSING SHALL BE 1.5 FEET ±0.1' FROM THE FACE OF THE GATE ARM IF THE GATE ARM EXTENDS ACROSS THE SIDEWALK. WHERE THERE IS NO PEDESTRIAN GATE, THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO THE RAILROAD CROSSING SHALL BE 15 FEET FROM THE NEAREST RAIL.
- ⑬ DO NOT INSTALL DETECTABLE WARNING FIELDS AT THE EDGES OF STREET-LEVEL PEDESTRIAN REFUGE ISLANDS IF A MINIMUM 2 FOOT CONCRETE SURFACE WITHOUT DETECTABLE WARNINGS (MEASURED IN THE DIRECTION OF PEDESTRIAN TRAVEL) CANNOT BE ACHIEVED.

LEGEND

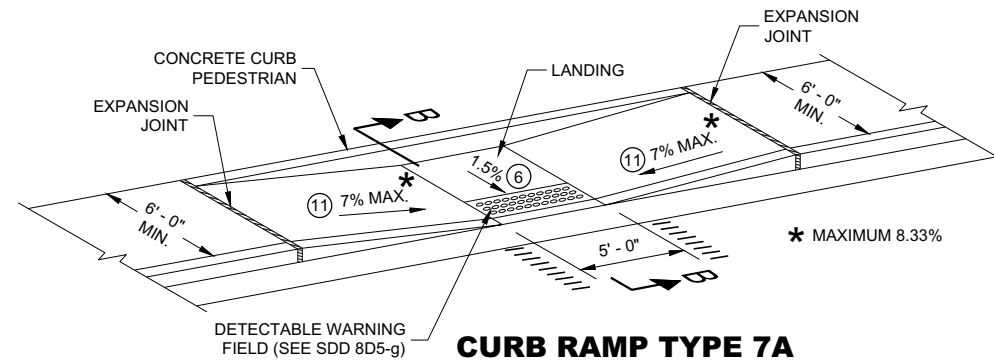
- ===== 1/2" EXPANSION JOINT SIDEWALK
- - - - - CONTRACTION JOINT FIELD LOCATED
- ||||||| PAVEMENT MARKING CROSSWALK (WHITE)



CURB RAMP TYPE 6

DETECTABLE WARNING AT ISLANDS

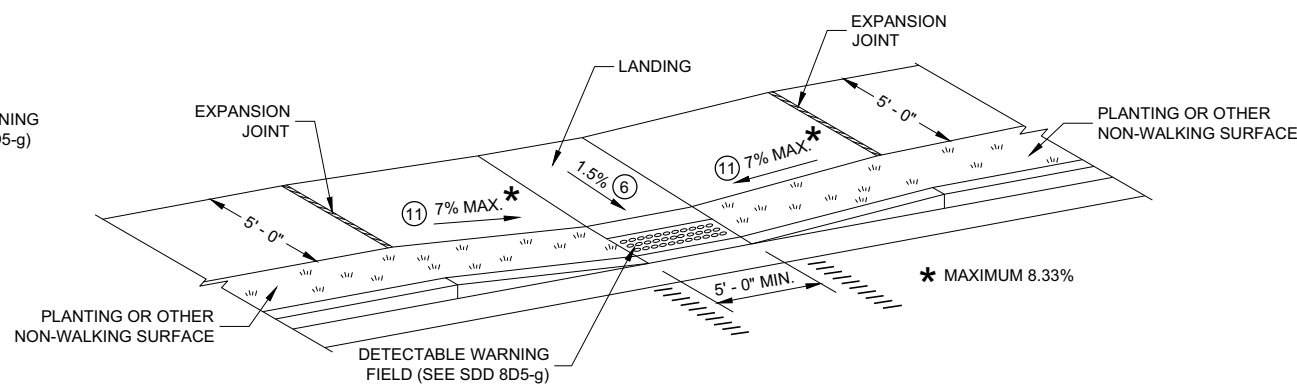
REFER TO GENERAL NOTES ② AND ③ FOR ALL ISLAND CURB RAMPS



CURB RAMP TYPE 7A

MID BLOCK CROSSING

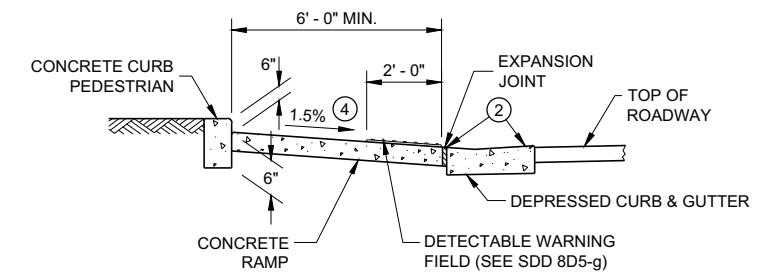
* MAXIMUM 8.33%



CURB RAMP TYPE 7B

MID BLOCK CROSSING

NOTE: THESE PARALLEL AND PARALLEL/PERPENDICULAR CURB RAMPS MAY BE USED AT INTERSECTIONS AND MID BLOCK LOCATIONS.

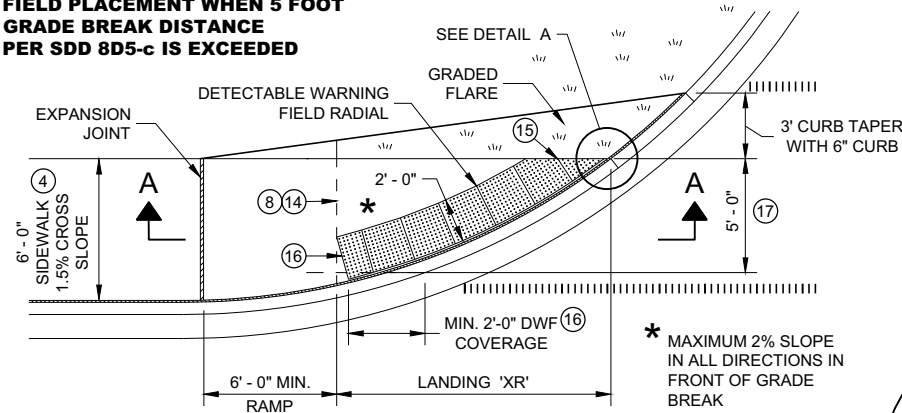


SECTION B - B FOR TYPE 7A

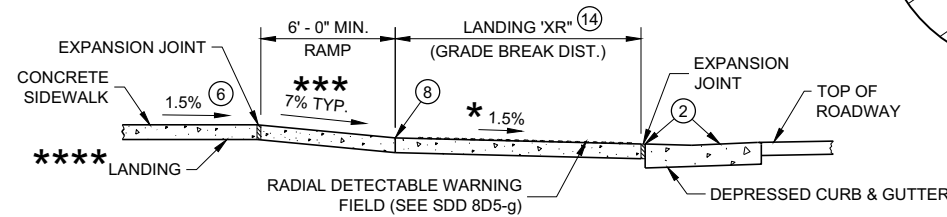
**CURB RAMPS
TYPE 5, 6, 7A, 7B & 8**

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RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-c IS EXCEEDED



**PLAN VIEW
CURB RAMP TYPE 4A1
(GRADE BREAK DISTANCE GREATER THAN 5 FEET)**

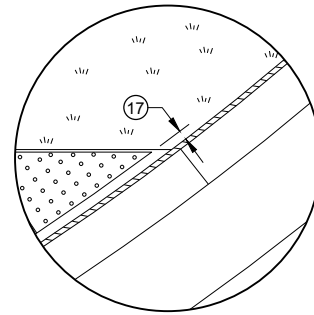


SECTION A - A FOR TYPE 4A1

**** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

*** MAXIMUM 8.33%

- LEGEND**
- 1/2" EXPANSION JOINT SIDEWALK
 - - - - - CONTRACTION JOINT SIDEWALK
 - ||||| PAVEMENT MARKING CROSSWALK (WHITE)

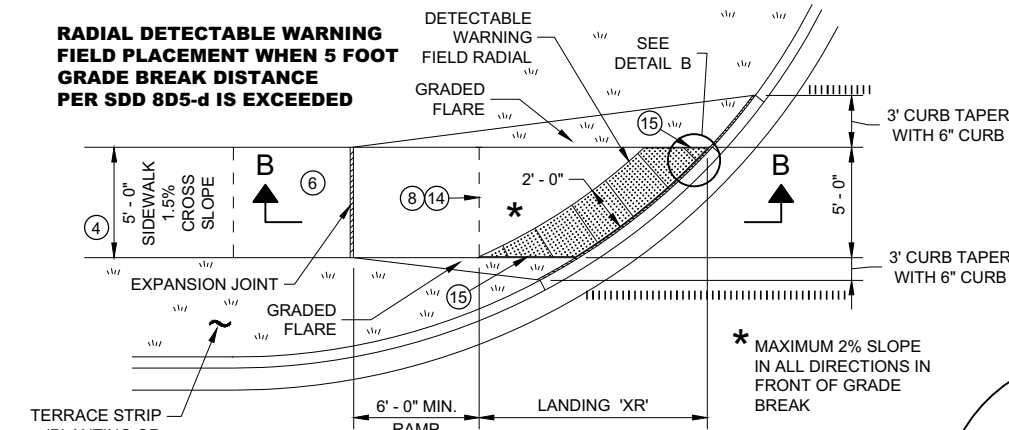


DETAIL A

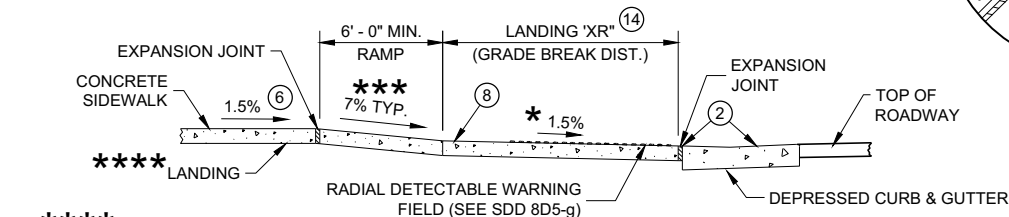
GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- APPLY RADIAL DETECTABLE WARNING PLACEMENT SIMILARLY FOR TYPE 4A AND 4A1 CURB RAMPS AND SIMILARLY FOR TYPE 4B AND 4B1 CURB RAMPS. TYPE 4A AND 4B RAMPS ARE NOT SHOWN.
- REFER TO SDD 8D5-g FOR ADDITIONAL RADIAL PLATE REQUIREMENTS.
- FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FILED ARE PROHIBITED.
- DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.
- 2 GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- 3 AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- 4 ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- 6 PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET BY 5 FEET.
- 8 PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- 14 CONSULT ENGINEER IF GRADE BREAK LOCATION (END OF LANDING DIMENSION "XR") REQUIRES FIELD ADJUSTMENT WHEN ESTABLISHING FINAL RADIAL DETECTABLE WARNING FIELD LOCATION.
- 15 FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN 1/8" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.
- 16 USE 1' X 2" RECTANGULAR END PLATE AT END OF TYPE 4A1 RAMP AND PROVIDE MINIMUM 2' - 0" DETECTABLE WARNING FIELD COVERAGE (IN DIRECTION OF PEDESTRIAN TRAVEL) ALONG THE ENTIRE CURB RAMP WIDTH.
- 17 A MAXIMUM 3 INCH CONCRETE BORDER WITH IS ALLOWABLE IN FROM OF RADIAL DETECTABLE WARNING FIELD FOR CONSTRUCTABILITY PURPOSES. CONCRETE BORDER WIDTH MAY VARY UP TO 1 INCH.

RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-d IS EXCEEDED



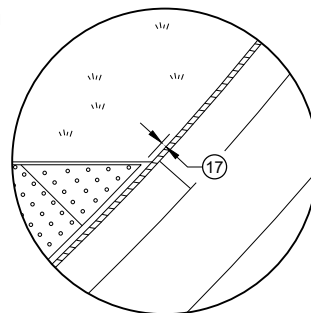
**PLAN VIEW
CURB RAMP TYPE 4B1
(GRADE BREAK DISTANCE GREATER THAN 5 FEET)**



SECTION B - B FOR TYPE 4B1

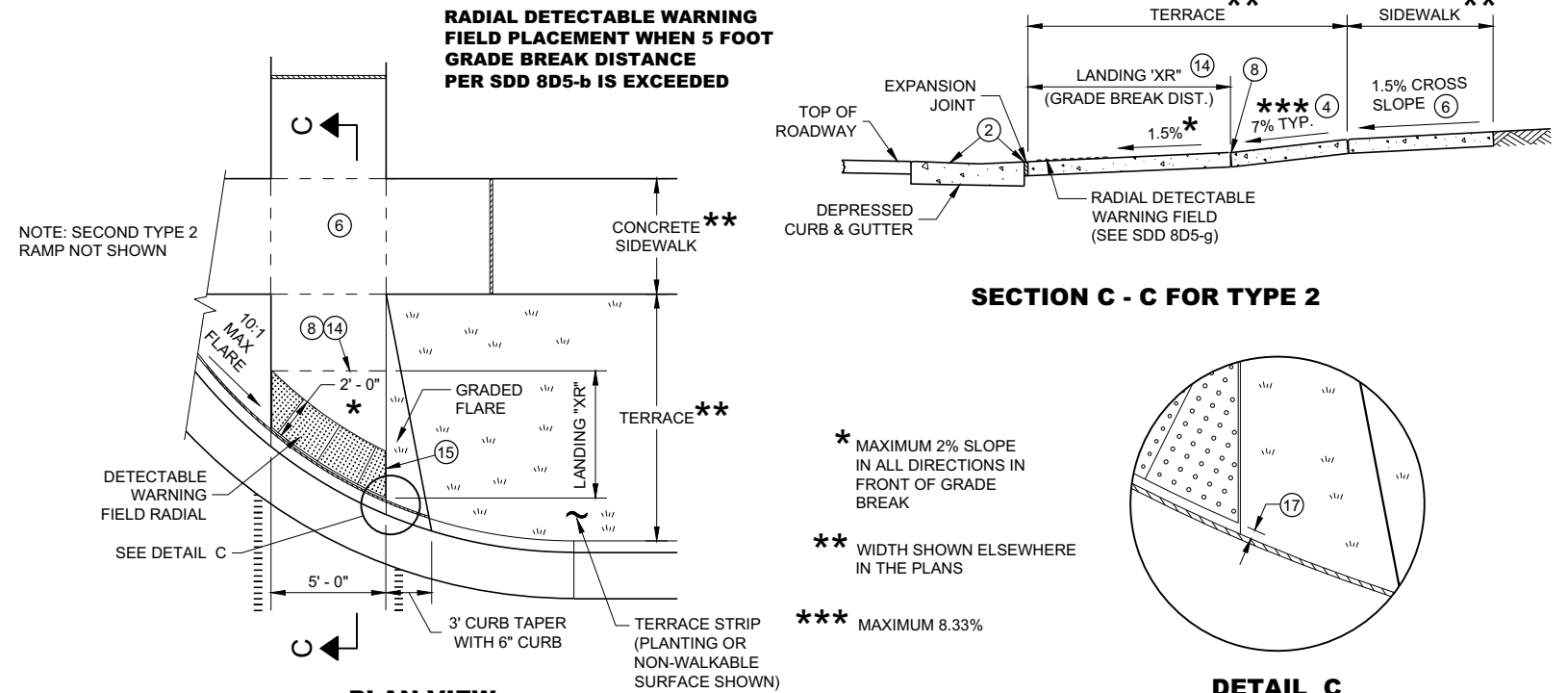
**** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

*** MAXIMUM 8.33%



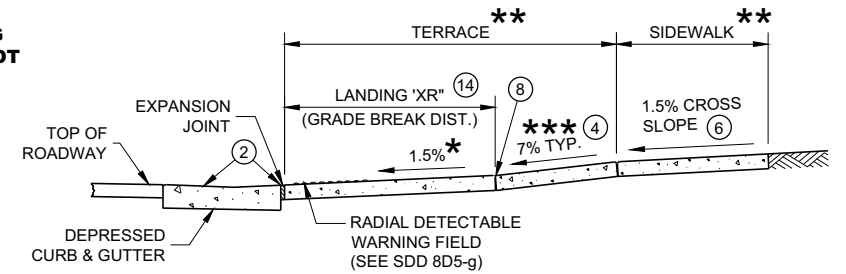
DETAIL B

RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-b IS EXCEEDED



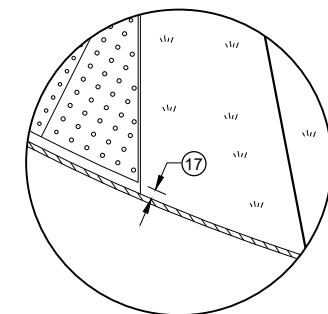
**PLAN VIEW
CURB RAMP TYPE 2
(GRADE BREAK DISTANCE GREATER THAN 5 FEET)
(ON LINE WITH SIDEWALK)**

NOTE: SECOND TYPE 2 RAMP NOT SHOWN



SECTION C - C FOR TYPE 2

- * MAXIMUM 2% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK
- ** WIDTH SHOWN ELSEWHERE IN THE PLANS
- *** MAXIMUM 8.33%



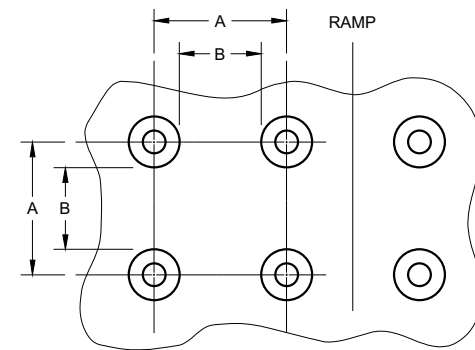
DETAIL C

**CURB RAMPS
RADIAL DETECTABLE WARNING
FIELD APPLICATIONS**

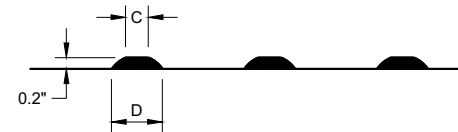
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

	MIN.	MAX.
A	1.6"	2.4"
B	0.65"	1.5"
C	*	*
D	0.9"	1.4"

* THE C DIMENSION IS 50% TO 65% OF THE D DIMENSION.

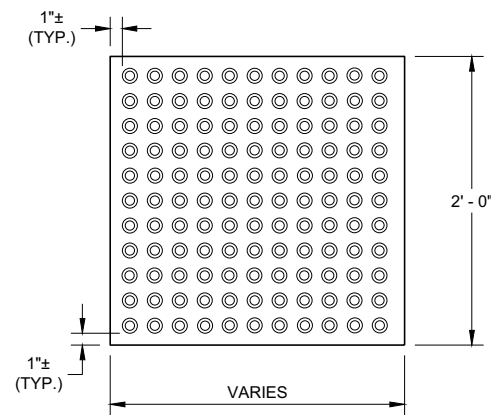


PLAN VIEW

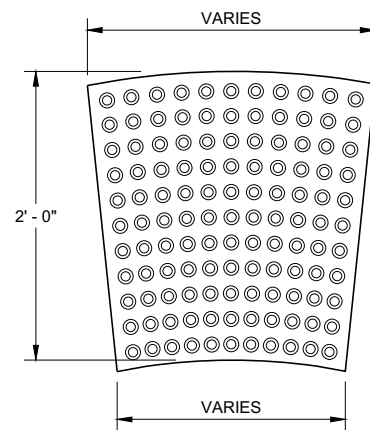


ELEVATION VIEW

**TRUNCATED DOMES
DETECTABLE WARNING PATTERN DETAIL**

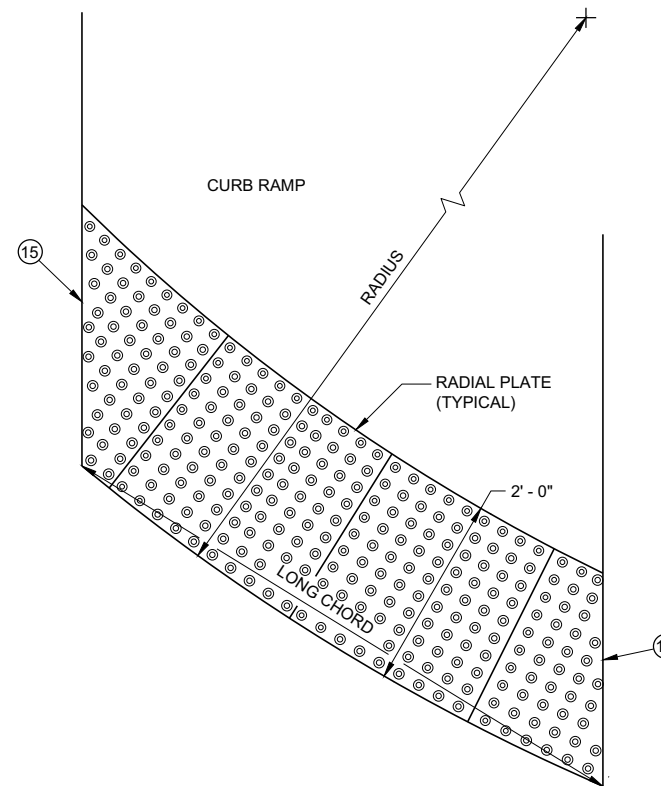


RECTANGULAR
PLATES

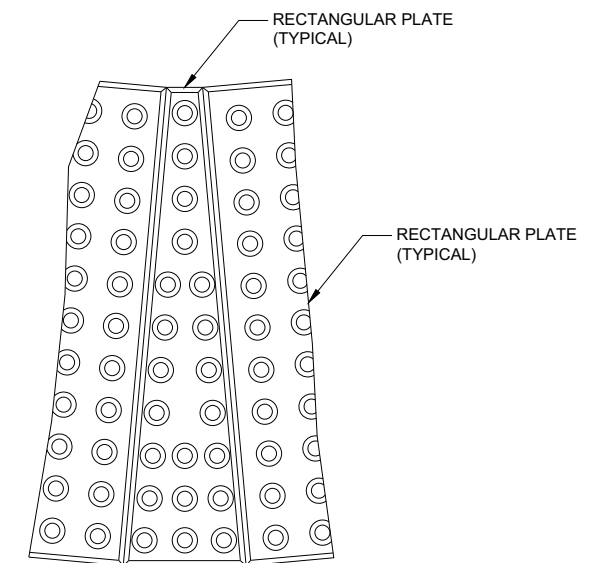


RADIAL
PLATES

PLAN VIEW
DETECTABLE WARNING FIELDS (TYPICAL)



PLAN VIEW
RADIAL DETECTABLE
WARNING FIELD ATTRIBUTES



PLAN VIEW
RADIAL WEDGE PLATE
CONNECTION DETAIL

GENERAL NOTES

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AT A CURB RAMP SHALL BE FROM THE SAME MANUFACTURER.

PLACE ALL DETECTABLE WARNING FIELD SYSTEMS IN ACCORDANCE TO THE MANUFACTURER'S RECOMMENDATION.

FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FIELD ARE PROHIBITED.

DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.

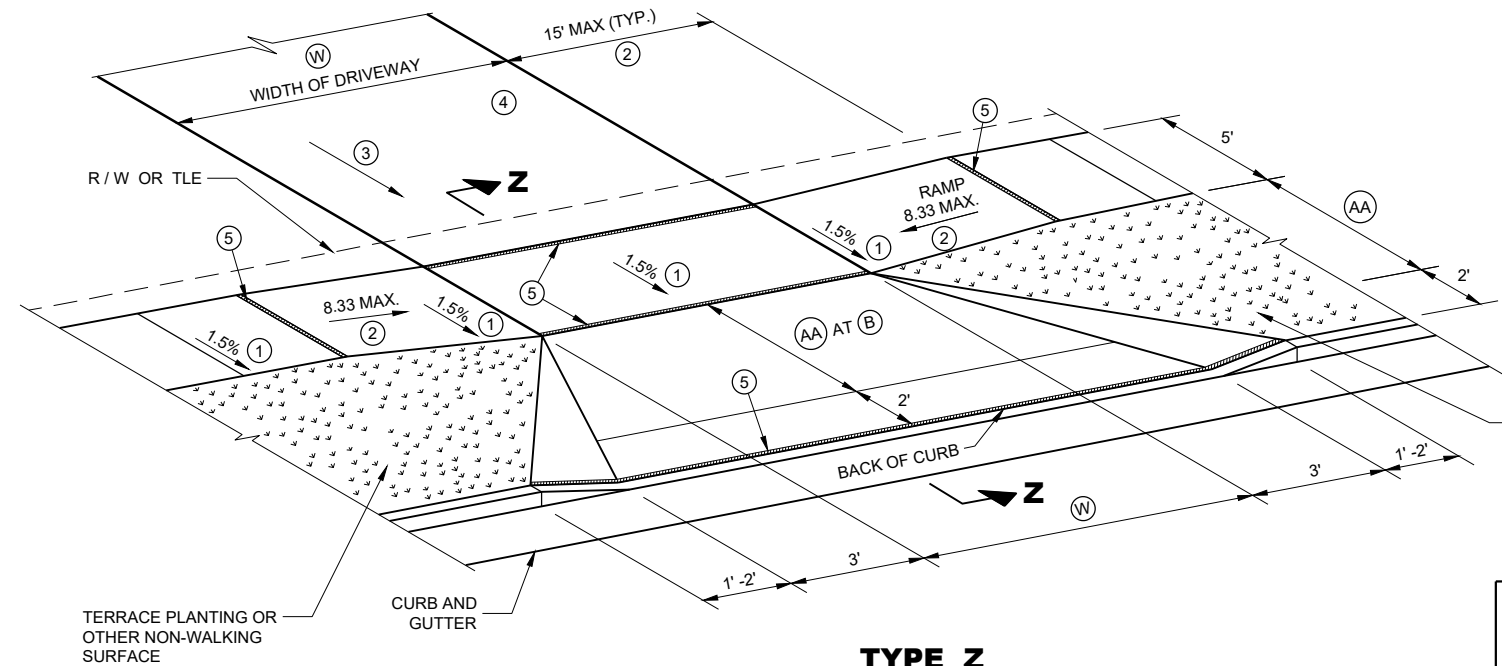
FOR RADIAL DETECTABLE WARNING FIELD APPLICATIONS WHERE STANDARD RADIAL PLATES ARE NOT AVAILABLE AT AN INTERSECTION CURB RADIUS, A COMBINATION OF SQUARE OR RECTANGULAR PLATES AND RADIAL PLATES MAY BE USED TO FORM RADIAL CONFIGURATION. RADIAL WEDGE PLATES IN COMBINATION WITH SQUARE PLATES ARE ALSO ACCEPTABLE. FOLLOW MANUFACTURER'S RECOMMENDATIONS.

REFER TO CONTRACT AND STANDARD SPECIFICATIONS FOR FIELD CUTTING REQUIREMENTS.

DO NOT EMBED IN CONCRETE ANY FIELD-CUT PLATES WITH CUT EDGES SHORTER THAN 6 INCHES. CONSULT WITH MANUFACTURER FOR RE-DRILLING AND ANCHORING REQUIREMENTS OF FIELD-CUT PLATES.

15 FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN 1/8" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.

CURB RAMPS RECTANGULAR AND RADIAL DETECTABLE WARNING PLATES	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2019 DATE	/S/ Rodney Taylor ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR
<small>FHWA</small>	



**TYPE Z
SIDEWALK WITH WIDER TERRACE
TERRACE VARIES 7 TO 12 FEET**

GENERAL NOTES

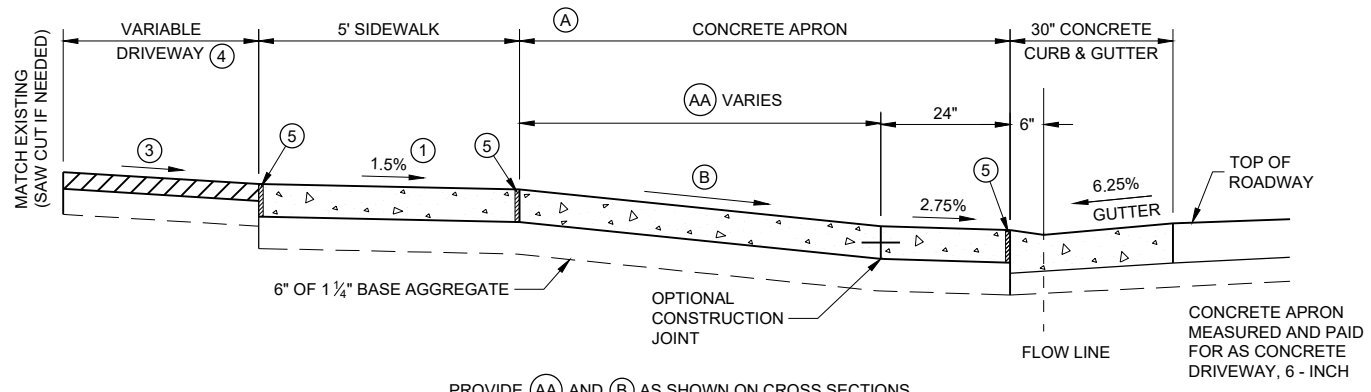
PROVIDE CONSTRUCTION JOINTS ALONG THE CENTER OF THE CONCRETE FOR DRIVEWAYS UNDER 20 FEET IN WIDTH AND AT THE THIRD POINTS OVER 20 FEET IN WIDTH.

- (W) IS SHOWN ON PLAN AND PROFILE SHEETS.
- OFFSETS, ELEVATIONS, AND PERCENT GRADE ARE SHOWN ON THE CROSS SECTIONS.
- ① CONSTRUCTION TOLERANCE OF 0.5%± FOR SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.
- ② THE SIDEWALK RAMP MAXIMUM RUNNING SLOPE SHALL NOT REQUIRE THE RAMP LENGTH TO EXCEED 15 FEET TO AVOID CHASING THE SLOPE INDEFINITELY WHEN CONNECTING TO STEEP GRADES. WHEN APPLYING THE 15 FOOT MAXIMUM LENGTH, THE RUNNING SLOPE OF THE SIDEWALK SHALL BE AS FLAT AS FEASIBLE AND NOT EXCEED THE LONGITUDINAL GRADE OF THE ROADWAY.
- ③ DRIVEWAY SLOPES: DESIRABLE MAXIMUM
10.5% UP AWAY FROM SIDEWALK (SAG)
8.5% DOWN AWAY FROM SIDEWALK (CREST)
ABSOLUTE MAXIMUM 15% FOR BOTH CREST AND SAG
- ④ DRIVEWAY TYPES
· 6-INCH CONCRETE DRIVEWAY PAVEMENT OVER 6-INCH BASE AGGREGATE
· 2-INCH TO 3-INCH ASPHALTIC SURFACE OVER 6-INCH BASE AGGREGATE
· 6-INCH BASE AGGREGATE (MAY BE INCREASED FOR CLAY SUBGRADES.)
- ⑤ ½" EXPANSION JOINT FILLER.

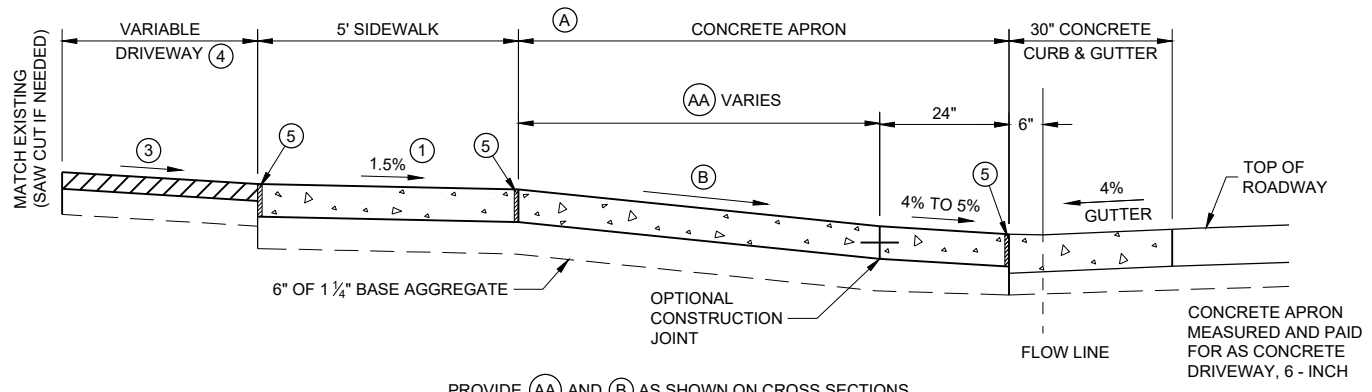
TABLE Z

(AA) FEET	(B) % 6.25% GUTTER	(B) % 4% GUTTER
4.5'	11.5%	9% TO 11.5%
5.5'	9% TO 11.5%	8% TO 11.5%
6.5'	8% TO 11.5%	6% TO 11.5%
7.5'	7% TO 11.5%	6% TO 11.5%
8.5'	6% TO 11.5%	5% TO 11.5%
9.5'	5% TO 11.5%	4% TO 11.5%

(W): 12' MIN. - 24' MAX. RESIDENTIAL AND NON-COMMERCIAL (PE & FE)
16' MIN. - 35' MAX. COMMERCIAL (CE)



6.25% GUTTER SLOPE



4% GUTTER SLOPE

NOTE: SIDEWALK MAY BE DEPRESSED IN DRIVEWAY AREAS FOR (B) VALUES NOT SHOWN IN TABLE Z.
SIDEWALK WITHIN THE LIMITS OF THE DRIVEWAY PAID FOR AS CONCRETE DRIVEWAY 6-INCH.
SEPARATE PAYMENT FOR BASE AGGREGATE WILL BE MADE.

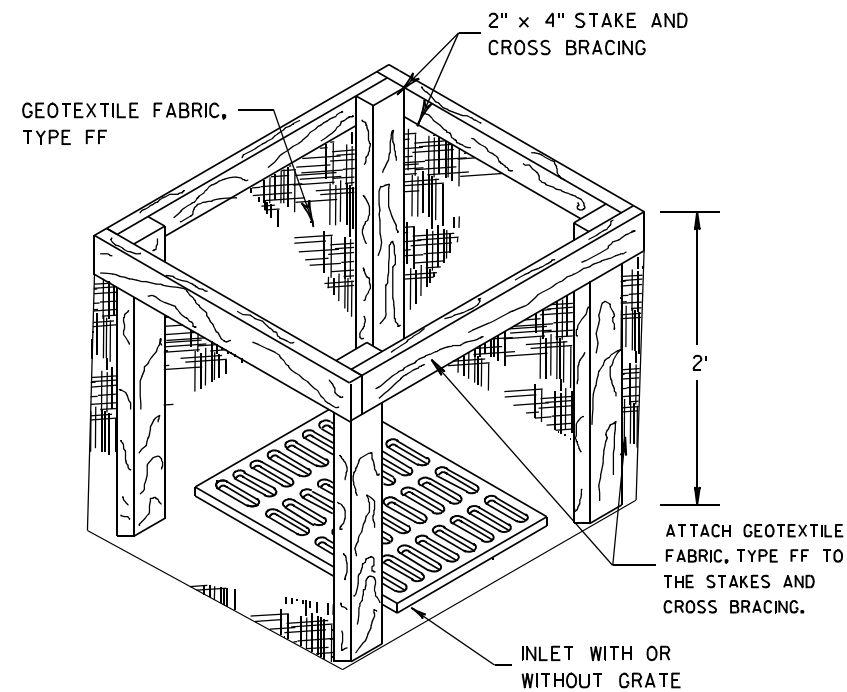
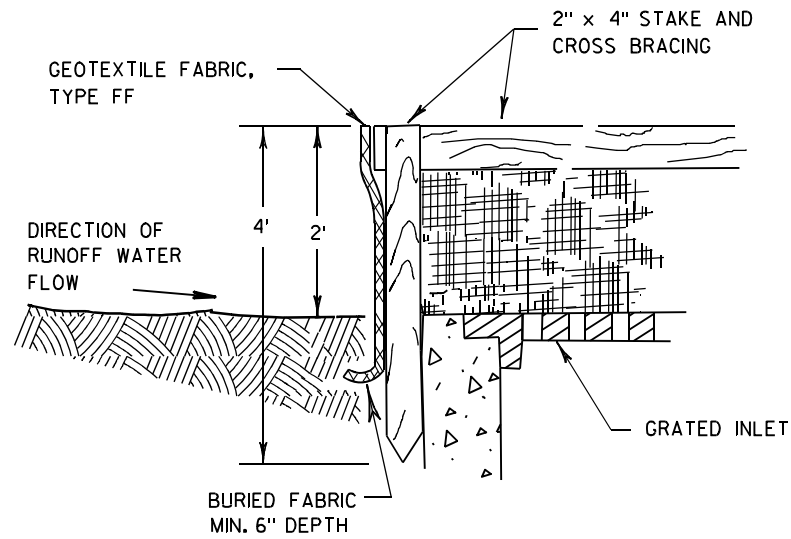
**SECTION Z - Z
DRIVEWAY DETAIL WITH CONCRETE CURB AND GUTTER
(URBAN AND SUBURBAN)**

**DRIVEWAY AND
SIDEWALK RAMPS
TYPE Z**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2022 DATE /S/ Rodney Taylor
ROADWAY STANDARDS DEVELOPMENT ENGINEER

FHWA



INLET PROTECTION, TYPE A

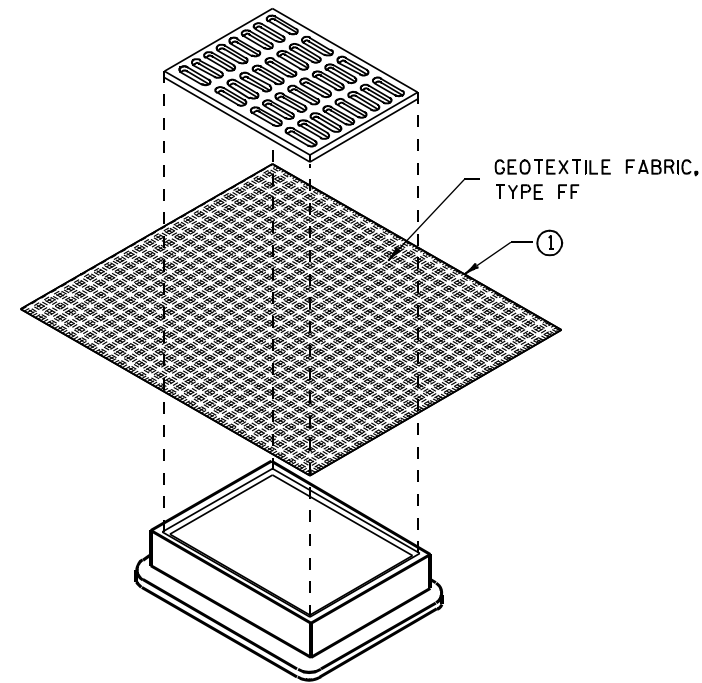
GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

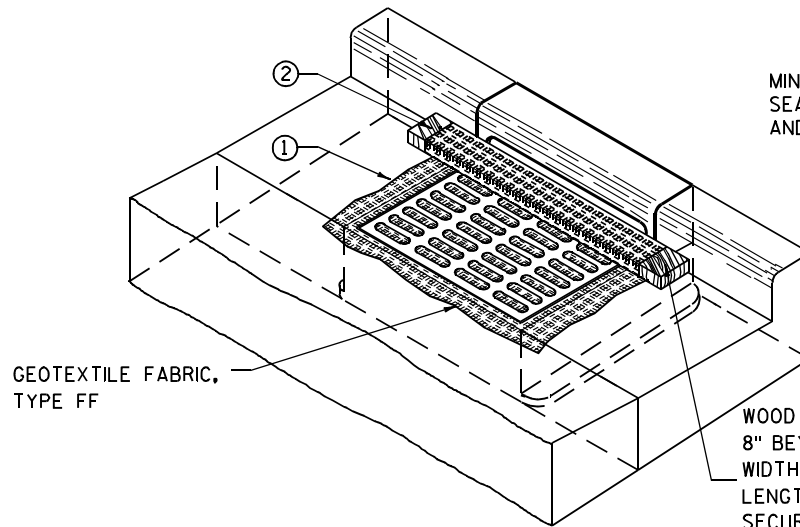
WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- ① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- ② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- ③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



**INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)**

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE C (WITH CURB BOX)

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

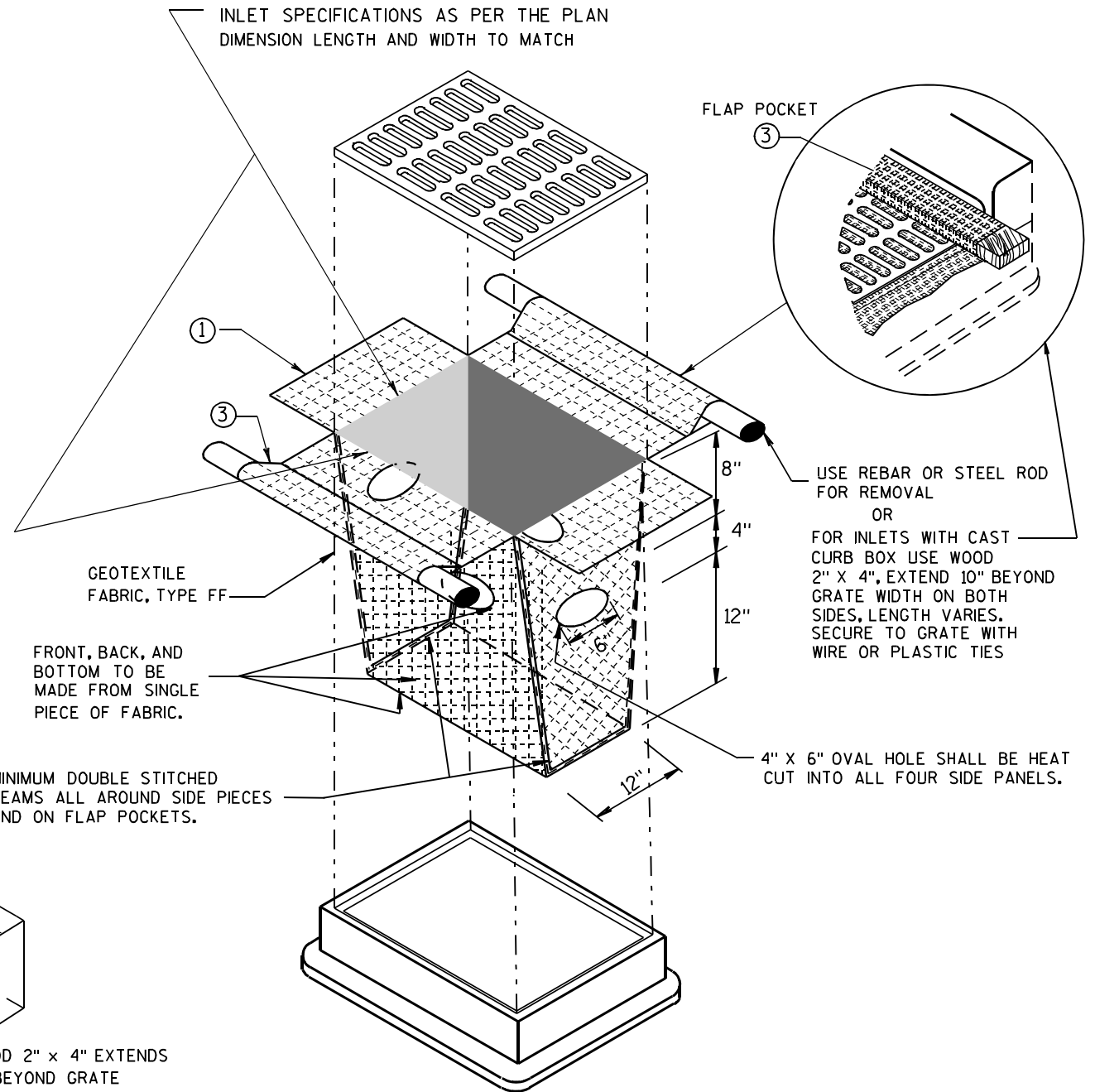
THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

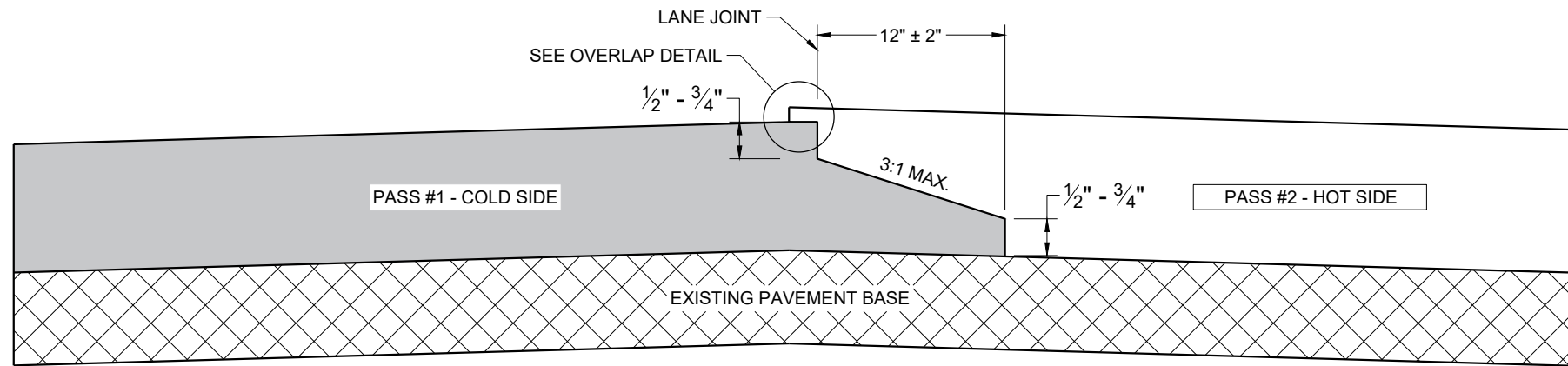
THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.



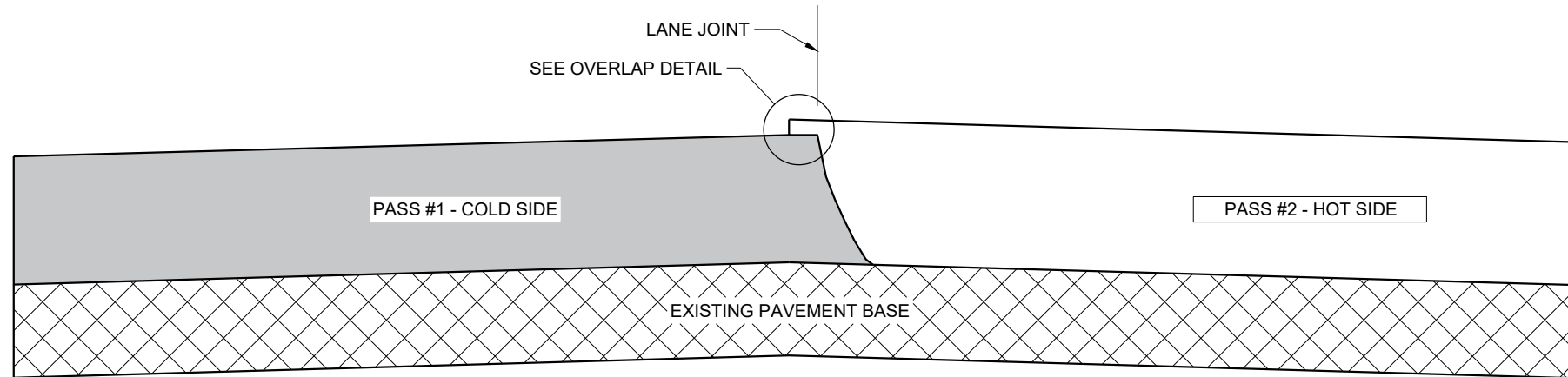
INLET PROTECTION, TYPE D

(CAN BE INSTALLED IN ANY INLET TYPE WITH OR WITHOUT A CURB BOX AS PER NOTE ②)

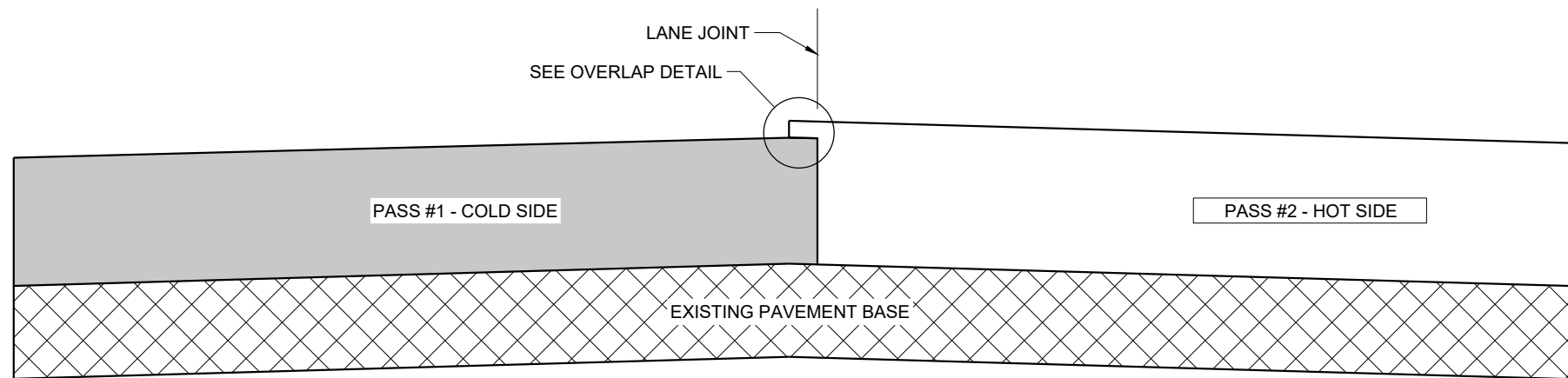
INLET PROTECTION TYPE A, B, C, AND D	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 10/16/02 DATE	/s/ Beth Connestra CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA	



**TYPICAL PAVEMENT CROSS SECTION
NOTCHED WEDGE JOINT**



**TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT**



**TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT (MILLED)**

GENERAL NOTES

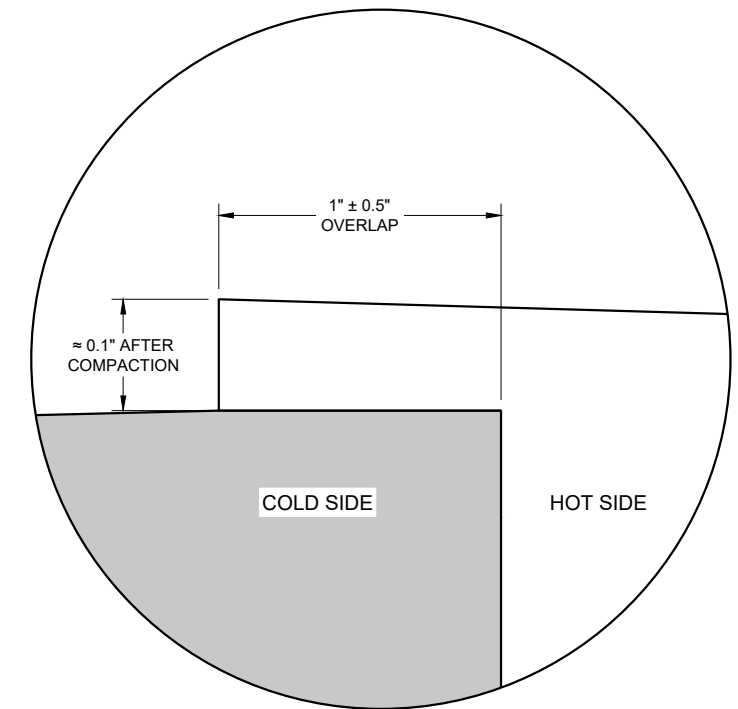
IN ADDITION TO THE DETAILS PROVIDED IN THIS DRAWING, CONFORM TO STANDARD SPECIFICATION 450.3.2.8 FOR WHEN A NOTCHED WEDGE JOINT IS REQUIRED AND FOR GENERAL JOINT CONSTRUCTION REQUIREMENTS.

FOR ALL LONGITUDINAL JOINTS, ENSURE THE PAVER SCREED OVERLAPS THE PREVIOUSLY PLACED PAVEMENT BY $1" \pm 0.5"$ AND THE HOT SIDE OF THE JOINT REMAINS HIGHER THAN THE COLD SIDE BY APPROXIMATELY 0.1" AFTER FINAL COMPACTION. (IT WILL BE FLUSH WHEN PAVING IN ECHELON.)

ONLY REMOVE THE LONGITUDINAL NOTCHED WEDGE JOINT FOR SMA PAVEMENT OR AS DIRECTED BY THE ENGINEER TO ADDRESS SPECIFIC LENGTHS OF JOINT DAMAGED BY TRAFFIC.

WHEN MILLING BACK OR REMOVING ANY LONGITUDINAL JOINT, LIMIT THE MATERIAL REMOVED TO 2" FROM THE TOP NOTCH OR FROM THE VERTICAL JOINT EDGE ON THE COLD SIDE OF THE JOINT.

USE LONGITUDINAL MILLED JOINT AS PLANS SHOW OR THE AS THE ENGINEER DIRECTS.



OVERLAP DETAIL (TYPICAL)

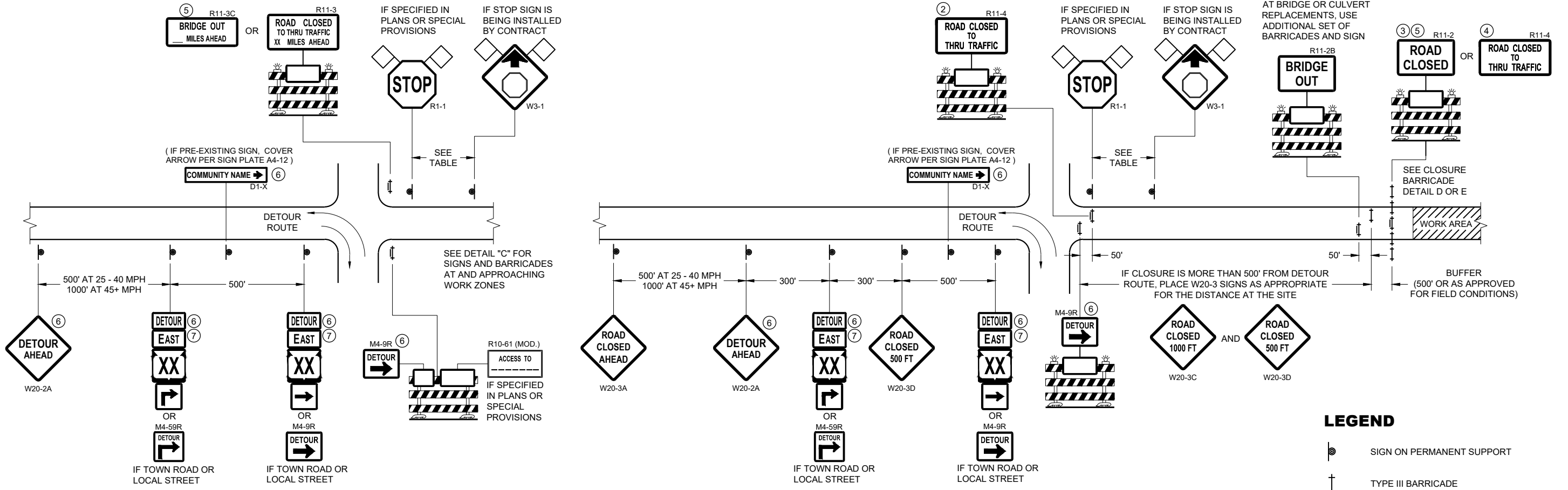
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6

SDD 13C19 - 03

SDD 13C19 - 03

HMA LONGITUDINAL JOINTS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED November 2020 DATE	/S/ Steven Hefel HMA PAVEMENT ENGINEER
FHWA	



**DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR**

WORK ZONE GREATER THAN OR EQUAL TO 1/2 MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)

**DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR**

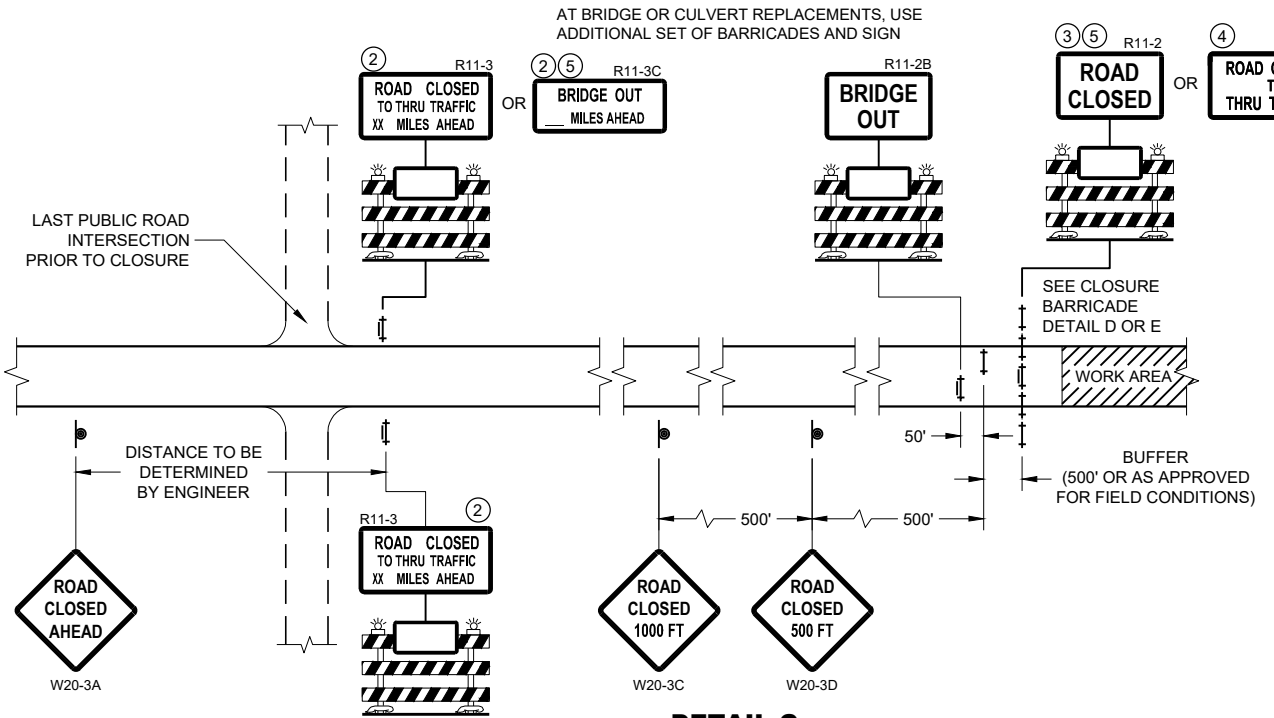
WORK ZONE LESS THAN 1/2 MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
- FLAGS, 16" X 16" MIN. (ORANGE)

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

- M4 - 8
- M3 - X
- M1 - 4
- M1 - 6
- M1 - 5A
- M05 - 1
- M06 - 1



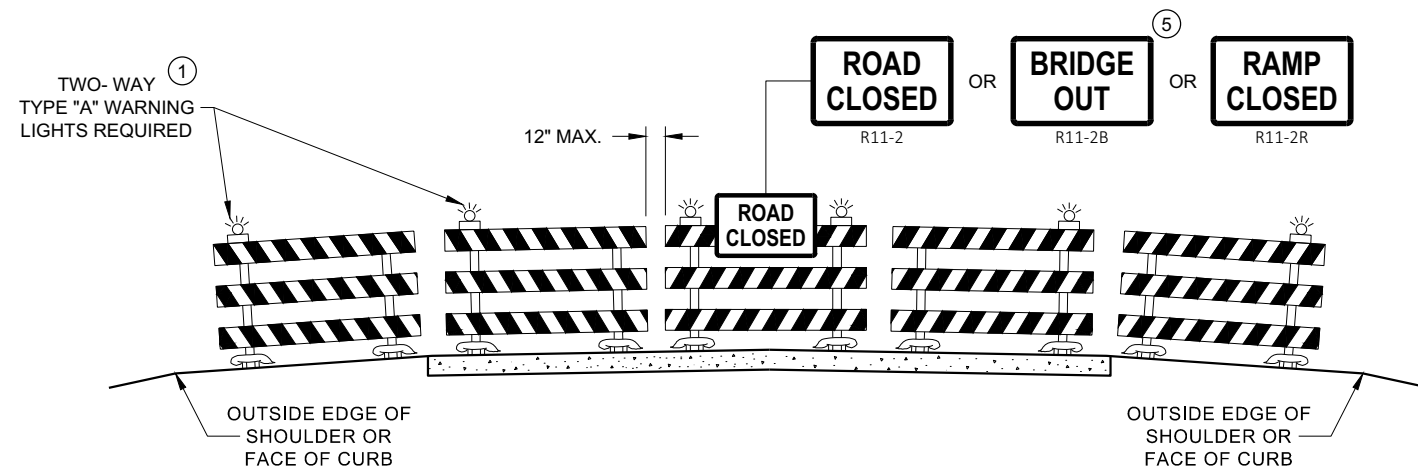
**DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR**

SEE SDD 15C2-SHEET "b"
FOR GENERAL NOTES
AND FOOTNOTES ① THROUGH ⑦

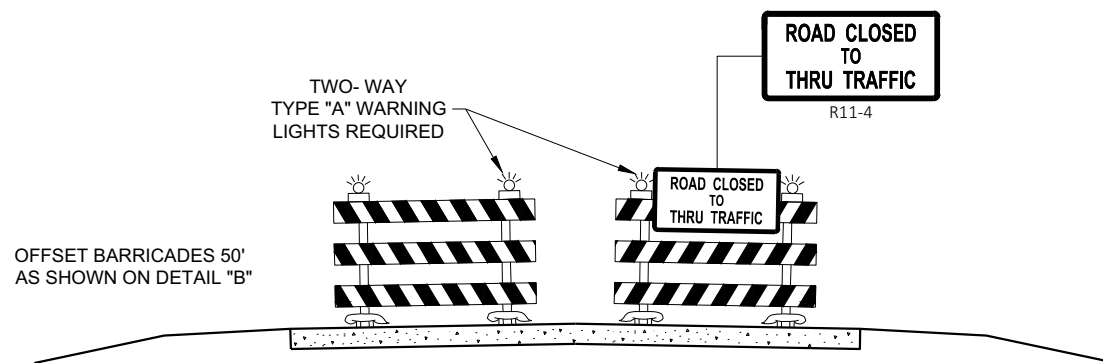
**BARRICADES AND SIGNS
FOR MAINLINE CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER



**DETAIL D
ROAD CLOSURE BARRICADE DETAIL
APPROACH VIEW**



**DETAIL E
LANE CLOSURE BARRICADE DETAIL
APPROACH VIEW**

SEE SDD 15C2 - SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW - INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11 - 2, R11 - 3, M4 - 9, R11 - 4, AND R10 - 61 SIGNS PLACED ON THE BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE RAIL OR BOTTOM RAILS.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

- R11 - 2 SHALL BE 48" X 30"
- R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60" X 30"
- M4 - 9 SHALL BE 30" X 24"
- M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1 - 1 SHALL BE 36" X 36"

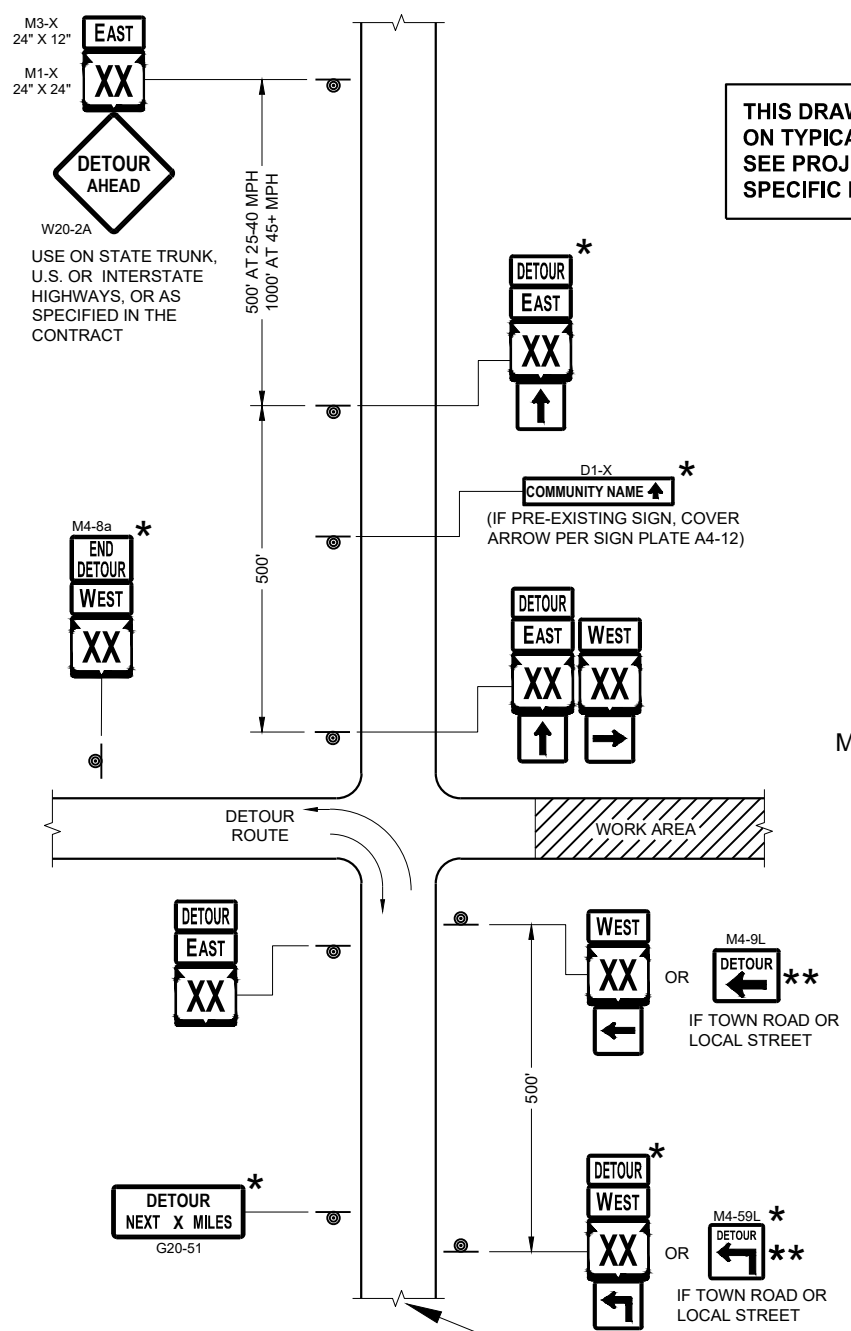
- ① TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT SPACING).
- ② THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.
- ③ FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".
- ④ FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".
- ⑤ FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.
- ⑥ INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.
- ⑦ "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

**BARRICADES AND SIGNS
FOR
VARIOUS CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA



THIS DRAWING PROVIDES GENERAL GUIDANCE ON TYPICAL DETOUR SIGN LAYOUT AND SPACING. SEE PROJECT DETOUR SIGNING SHEETS FOR SPECIFIC DETAILS FOR EACH PROJECT.

LEGEND

- SIGN ON PERMANENT SUPPORT
- WORK AREA
- M4 - 8
- M3 - X
- M1 - 4
- M1 - 6
- M1 - 5A
- M05 - 1
- M06 - 1
- M06 - 1

GENERAL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. MODIFY EXISTING SIGNS WHERE POSSIBLE.

THE SPACING BETWEEN TRAFFIC CONTROL AND DETOUR SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

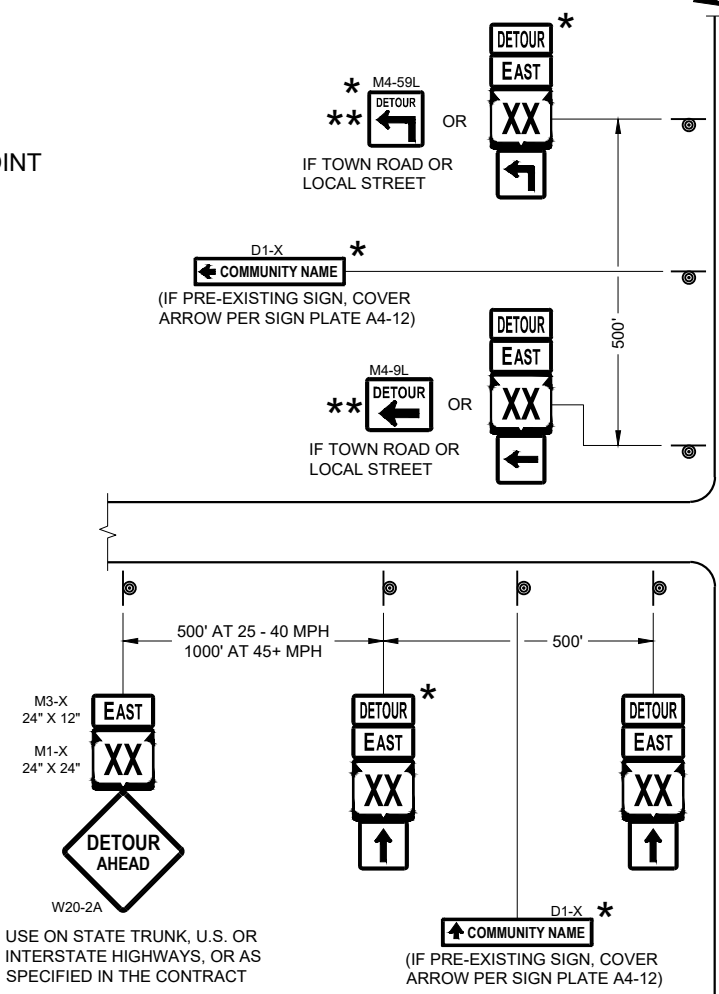
"MO" SIGNS ARE THE SAME AS "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGN SIZES SHALL BE AS FOLLOWS:

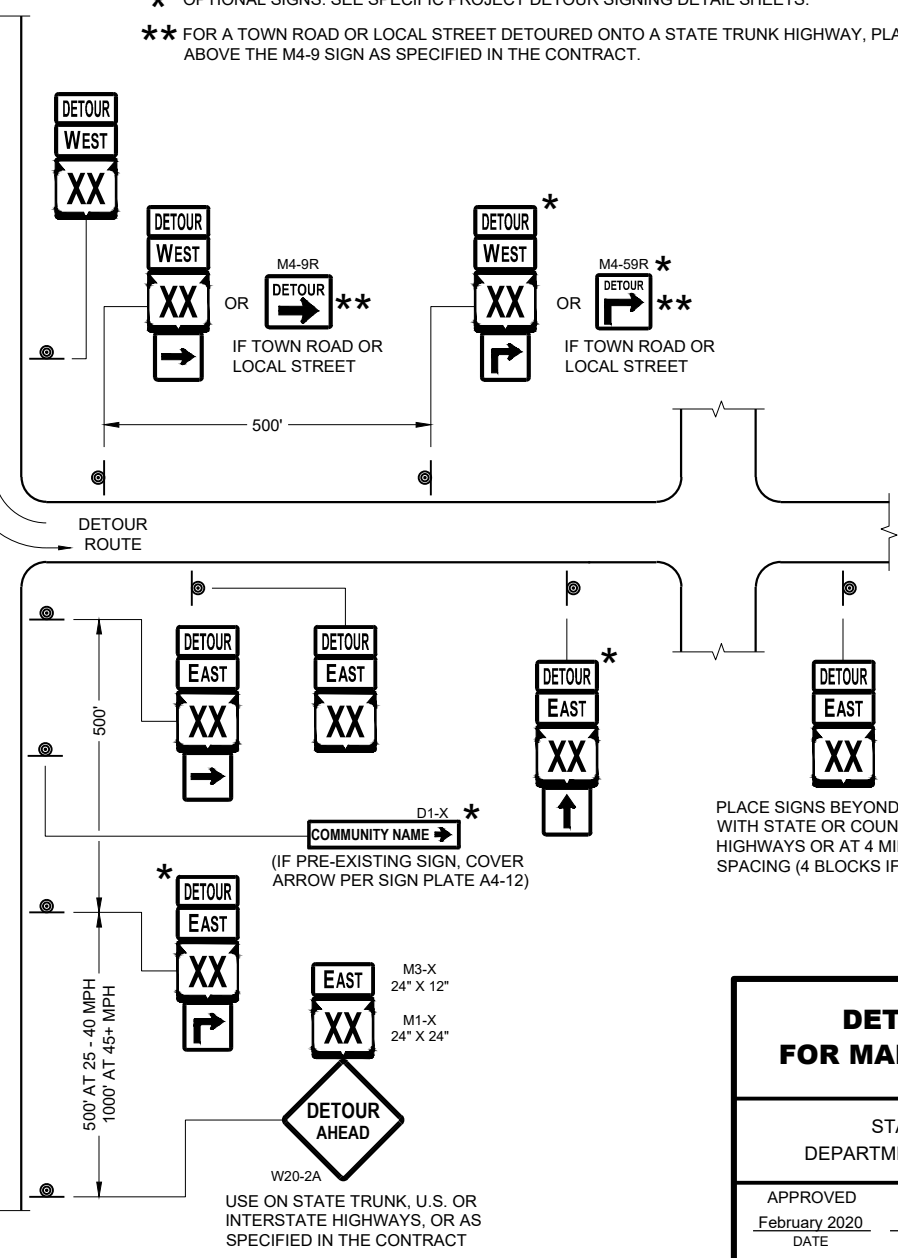
- M3-X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4-8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1-4, M1-5A AND M1-6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- M05-1 AND M06-1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- M4-9 AND M4-59 SHALL BE 30" X 24"
- M4-8a SHALL BE 24" X 18"
- G20-51 SHALL BE 60" X 24"
- W20-2A SHALL BE 48" X 48"
- D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.

- * OPTIONAL SIGNS. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS.
- ** FOR A TOWN ROAD OR LOCAL STREET DETOURED ONTO A STATE TRUNK HIGHWAY, PLACE A ROAD NAME PLAQUE ABOVE THE M4-9 SIGN AS SPECIFIED IN THE CONTRACT.

MATCH POINT



**DETAIL F
DETOUR SIGNING**



**DETOUR SIGNING
FOR MAINLINE CLOSURES**

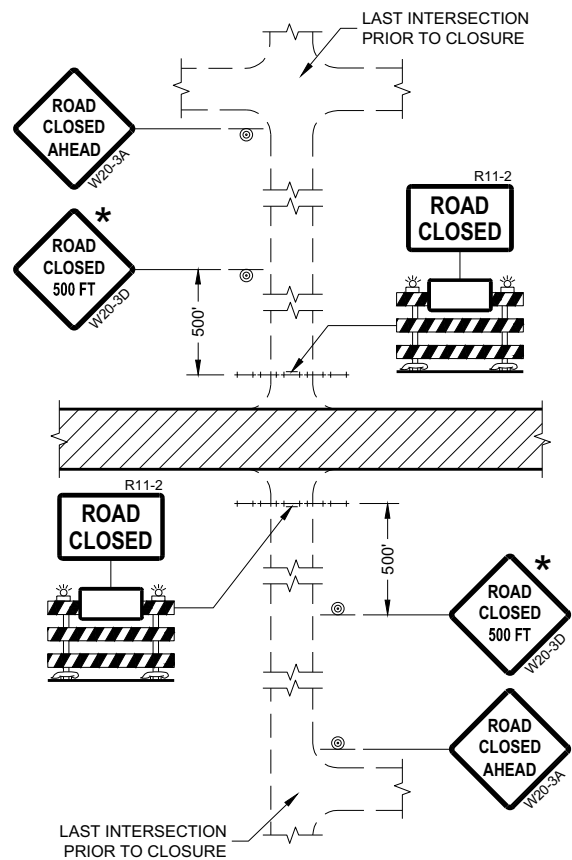
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

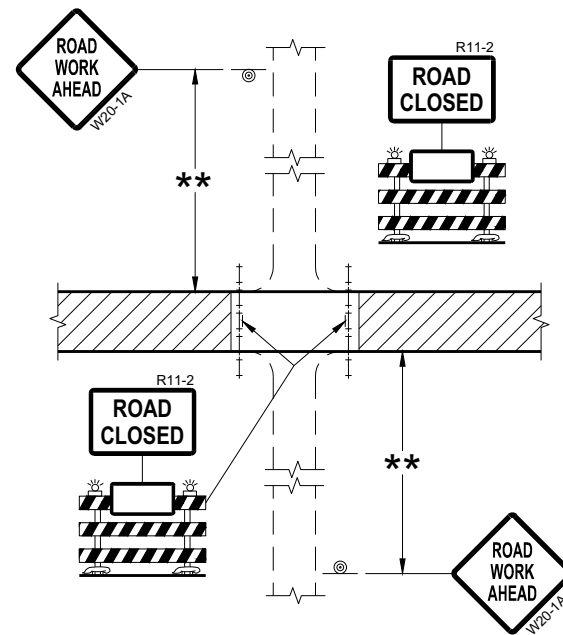
FHWA

SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS AND DETAIL A OR B ON SDD SHEET 15C02 - SHEET "a"

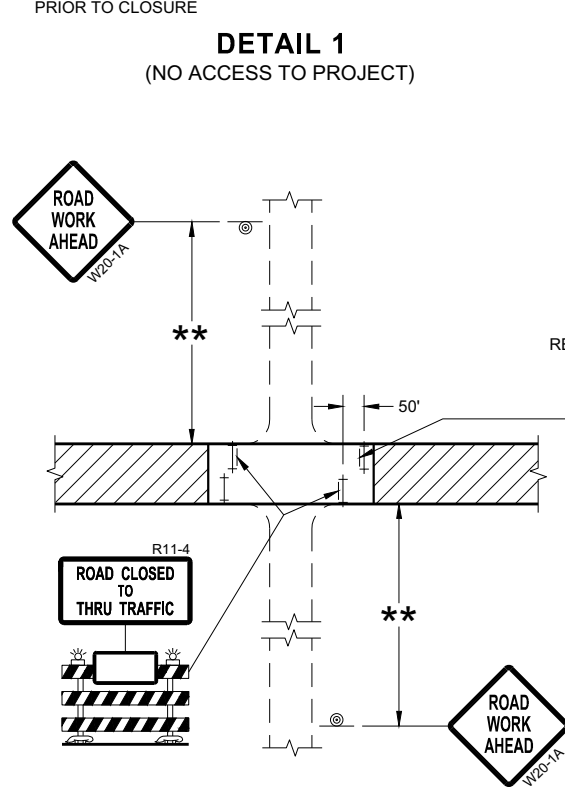
PLACE SIGNS BEYOND INTERSECTIONS WITH STATE OR COUNTY TRUNK HIGHWAYS OR AT 4 MILE MAXIMUM SPACING (4 BLOCKS IF URBAN AREA)



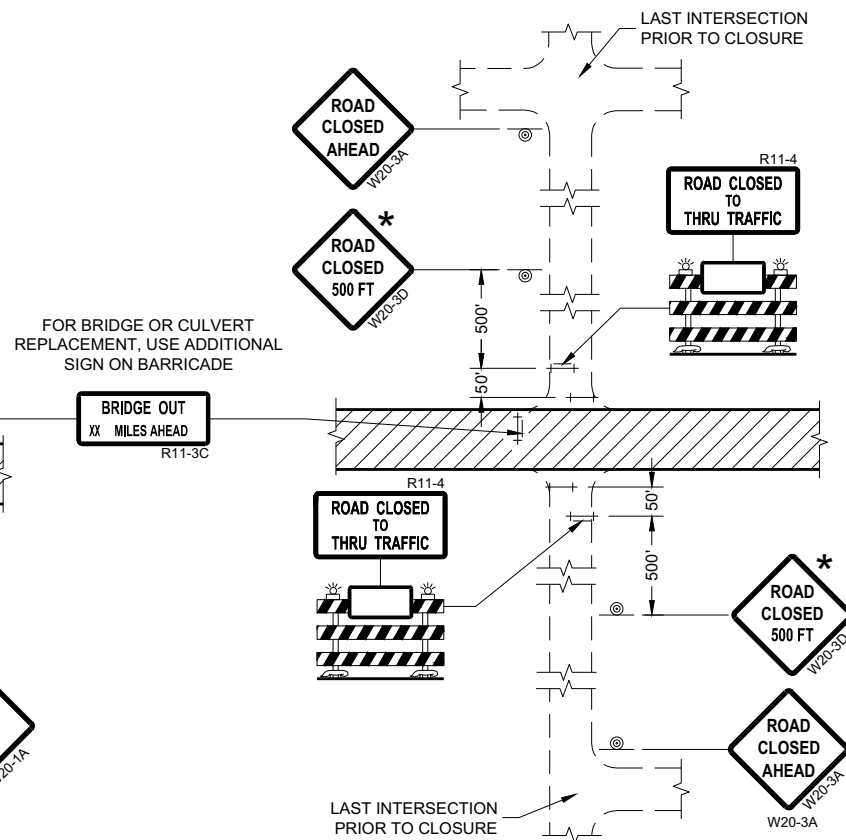
DETAIL 1
(NO ACCESS TO PROJECT)



DETAIL 2
(PUBLIC CROSS-TRAFFIC MAINTAINED.
NO ACCESS TO PROJECT)



DETAIL 3
(PUBLIC CROSS-TRAFFIC MAINTAINED.
CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)



DETAIL 4
(CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE (500 FEET DESIRABLE) TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3, AND R11-4 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
R11-2 SHALL BE 48" X 30".
R11-4 AND R11-3 SHALL BE 60" X 30".

- * OMIT THE "ROAD CLOSED 500 FT." SIGN IF THE LAST INTERSECTION IS 500 FEET OR LESS FROM THE WORK ZONE.
- ** 500' MAX. OR AT LAST INTERSECTION, WHICHEVER IS CLOSEST.

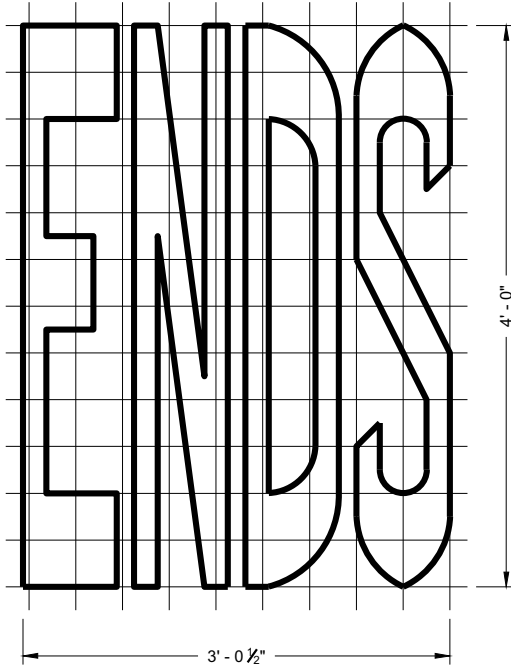
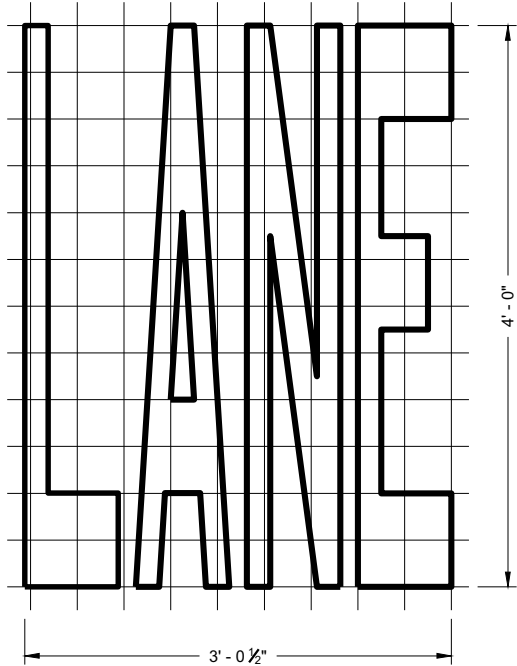
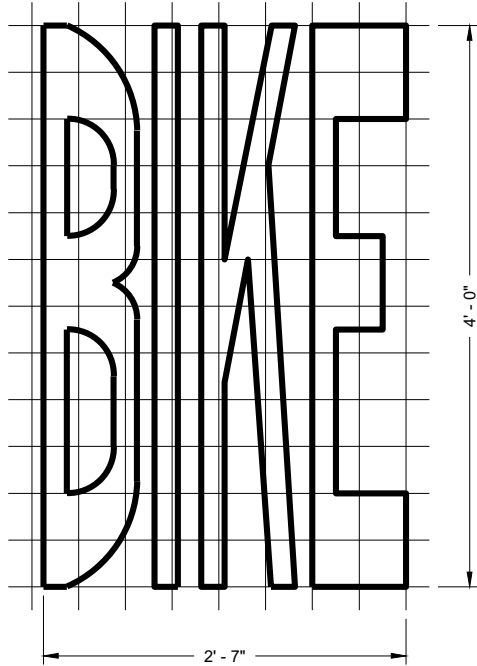
LEGEND

- ⊙ SIGN ON PERMANENT SUPPORT
- ⊥ TYPE III BARRICADE
- ⊥ TYPE III BARRICADE WITH ATTACHED SIGN
- ⚡ TYPE "A" WARNING LIGHT (FLASHING)
- ▨ WORK AREA

**BARRICADES AND SIGNS
FOR
SIDEROAD CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

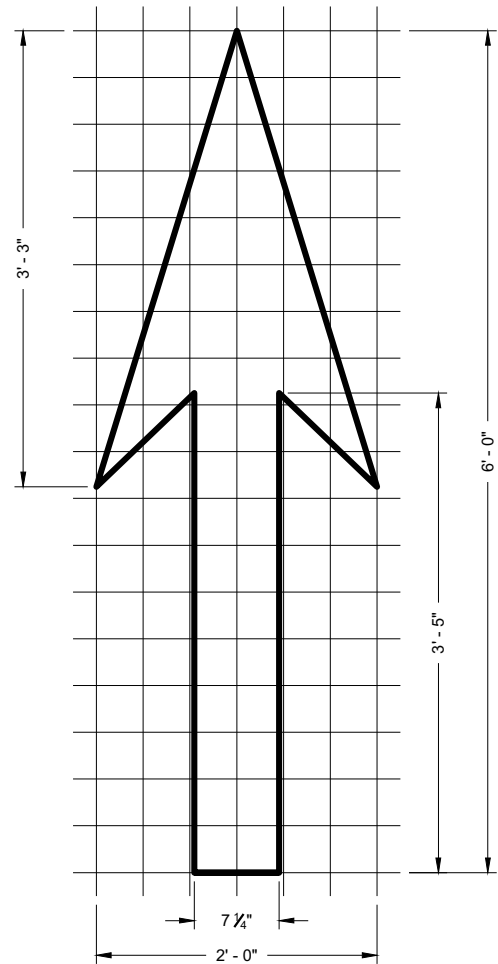
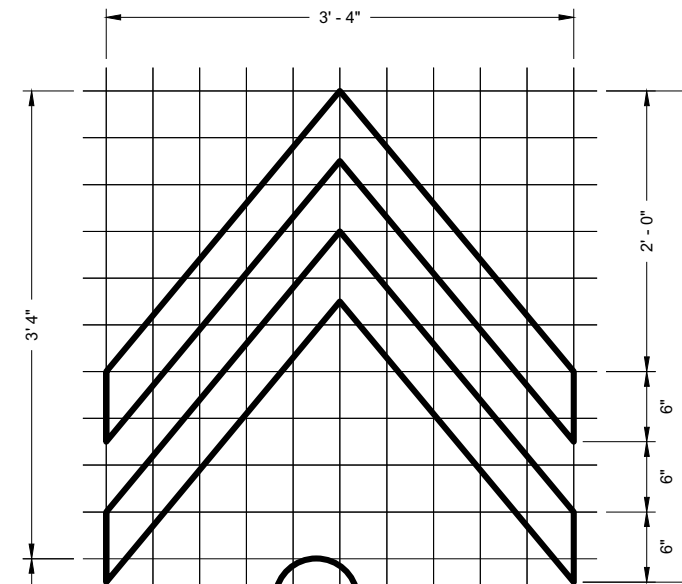
APPROVED
July 2018 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER



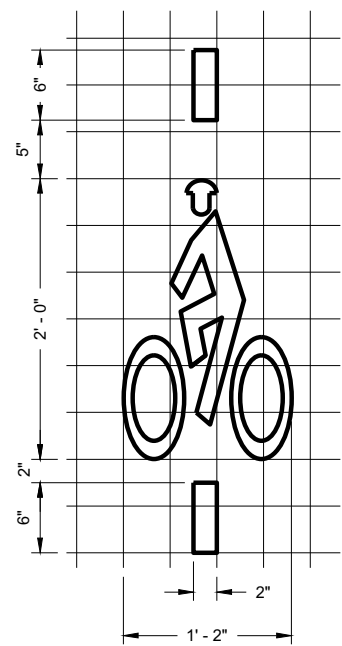
BIKE LANE WORDS

GENERAL NOTES

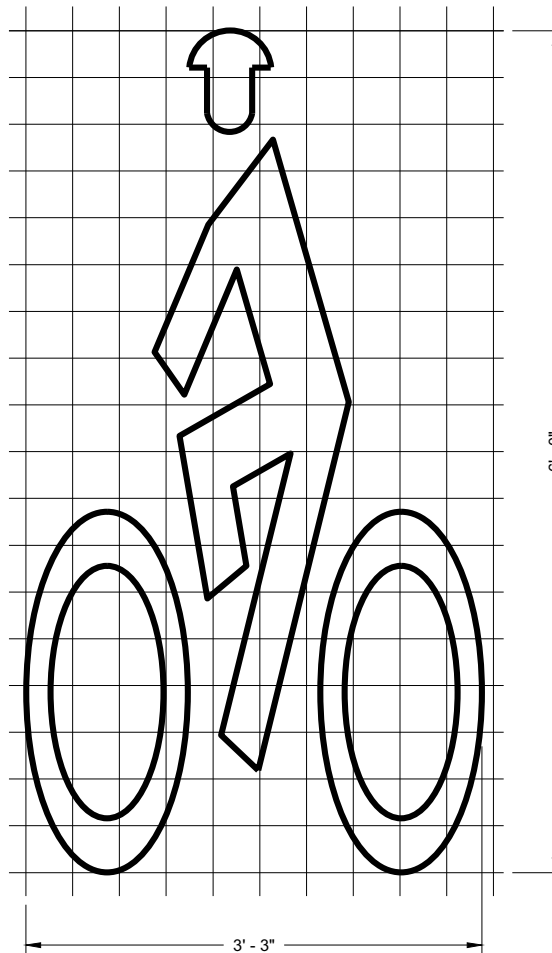
DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.



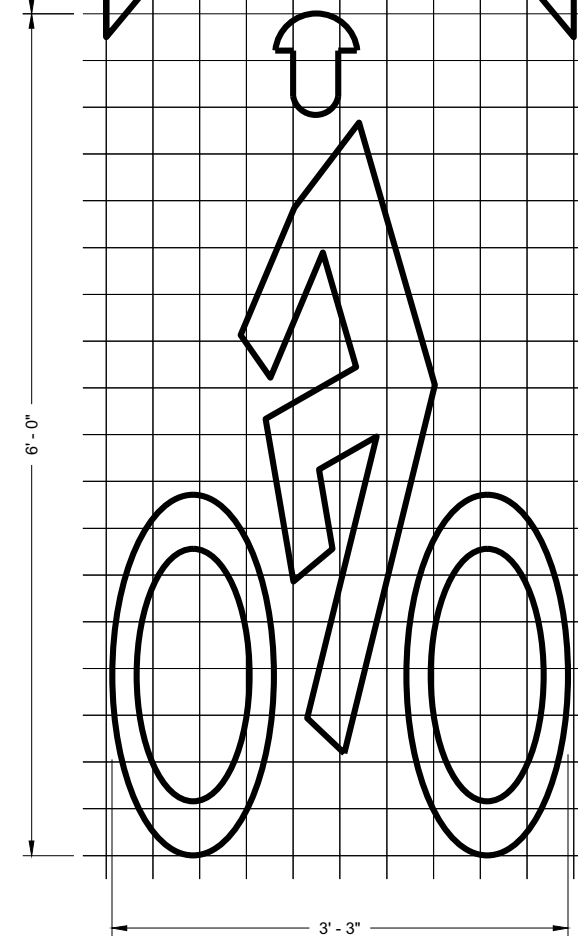
BIKE LANE ARROW



BIKE DETECTOR PAVEMENT MARKING



BIKE LANE SYMBOL



BIKE LANE SYMBOL FOR SHARED LANE

6

6

PAVEMENT MARKING FOR BIKE LANES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2019 /S/ Matthew Rauch
DATE STATE SIGNING AND MARKING
ENGINEER




FHWA

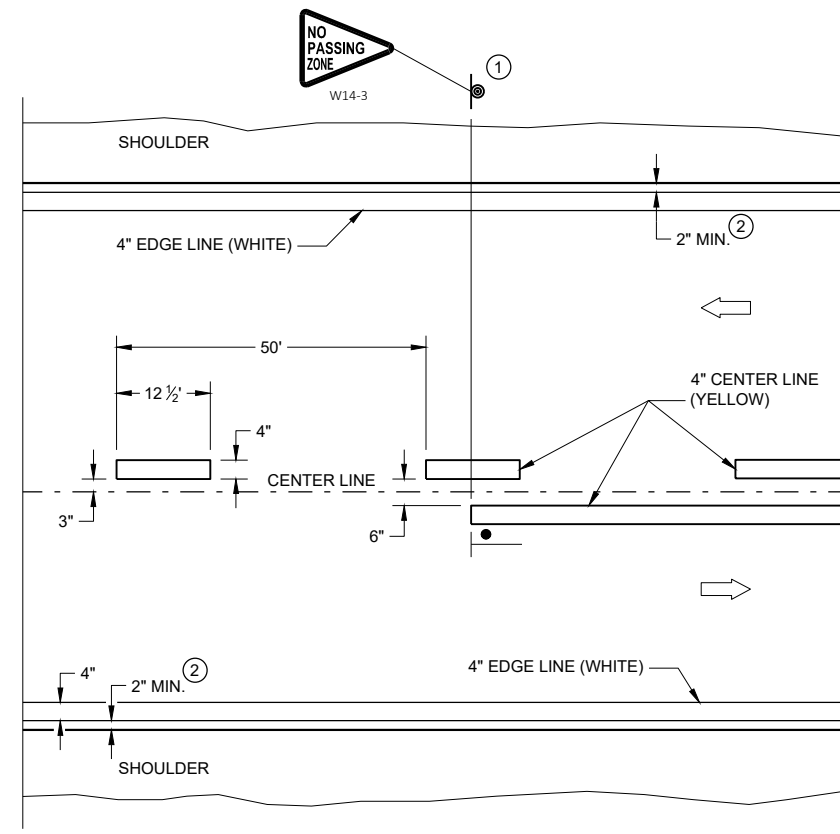
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

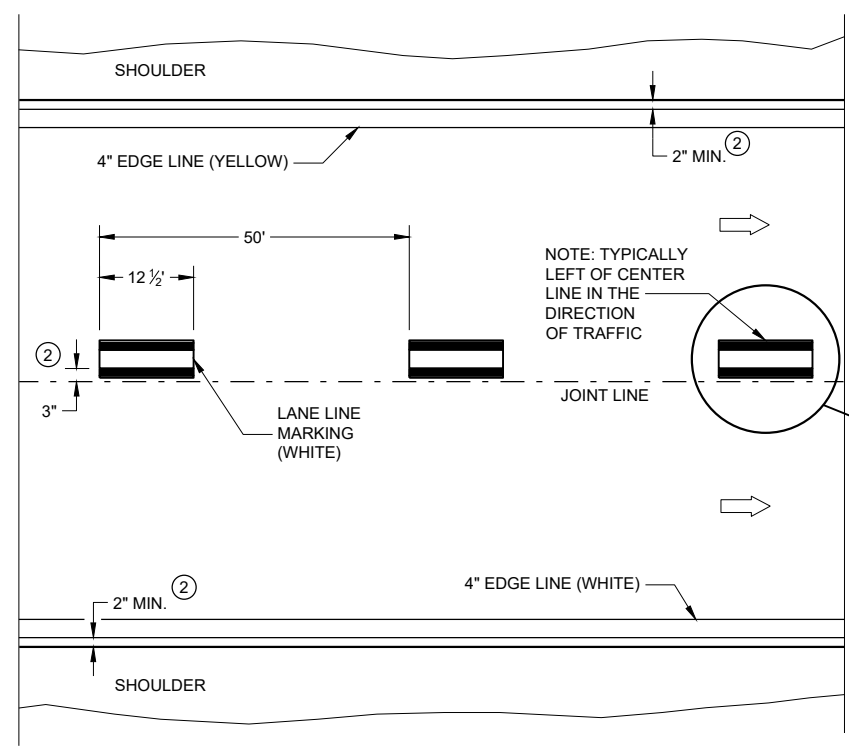
- ① LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING
- ② MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

LEGEND

-  "T" MARKING
-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC

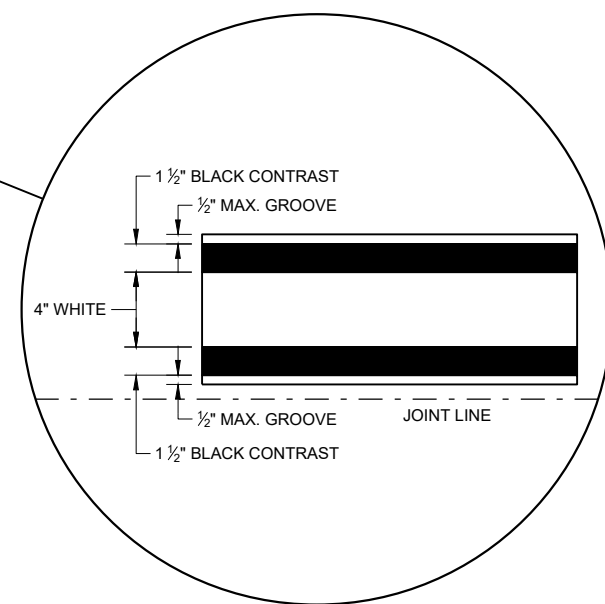


TWO WAY TRAFFIC



ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING



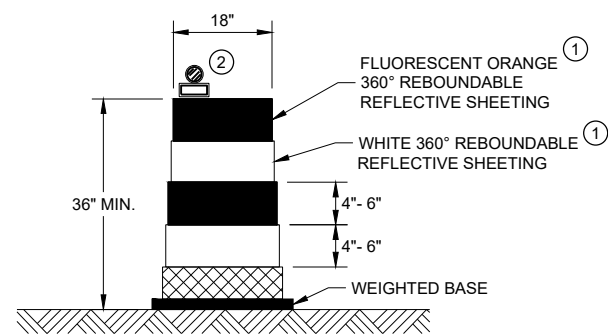
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6

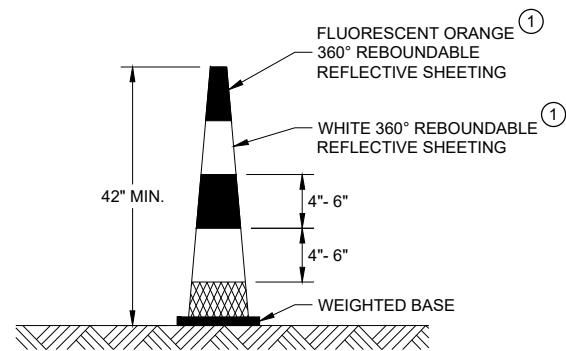
SDD 15C08 - 21a

SDD 15C08 - 21a

PERMANENT LONGITUDINAL PAVEMENT MARKINGS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2022 DATE	/S/ Jeannie Silver STATEWIDE SIGNING AND MARKING ENGINEER
<small>FHWA</small>	

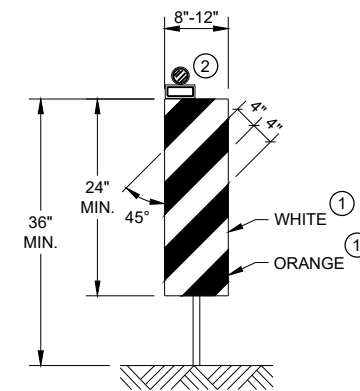


DRUM



42" CONE

DO NOT USE IN TAPERS
 1/2 SPACING OF DRUMS

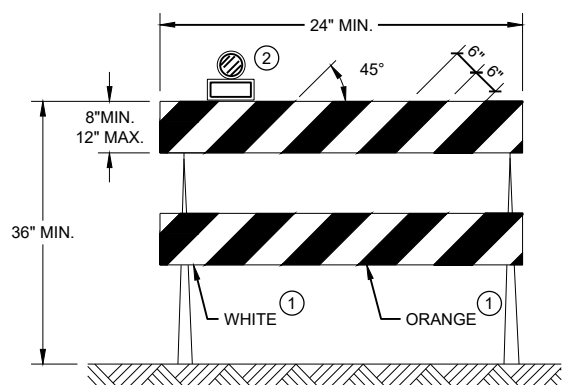


VERTICAL PANEL

THE STRIPES SHALL SLOPE DOWNWARD TO THE TRAFFIC SIDE FOR CHANNELIZATION.

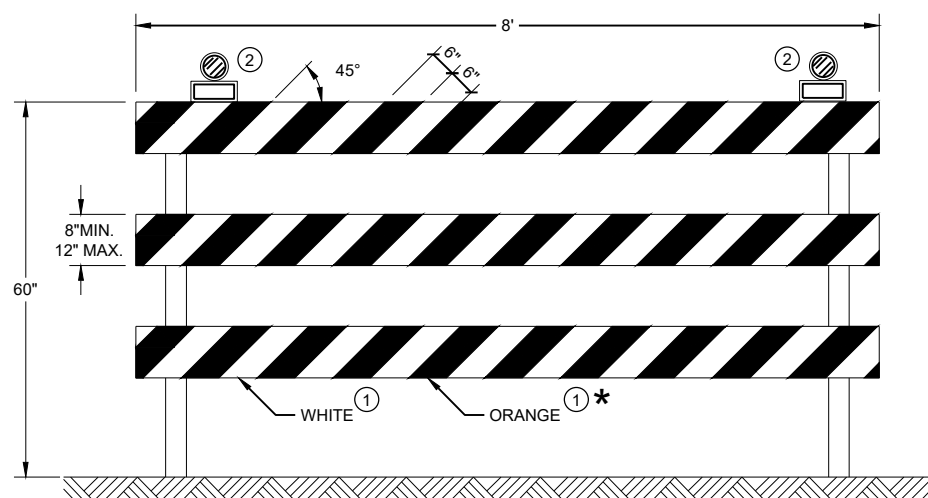
GENERAL NOTES

- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.



TYPE II BARRICADE

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED. ALL STRIPES SHALL SLOPE DOWNWARD TO THE TRAFFIC SIDE FOR CHANNELIZATION.

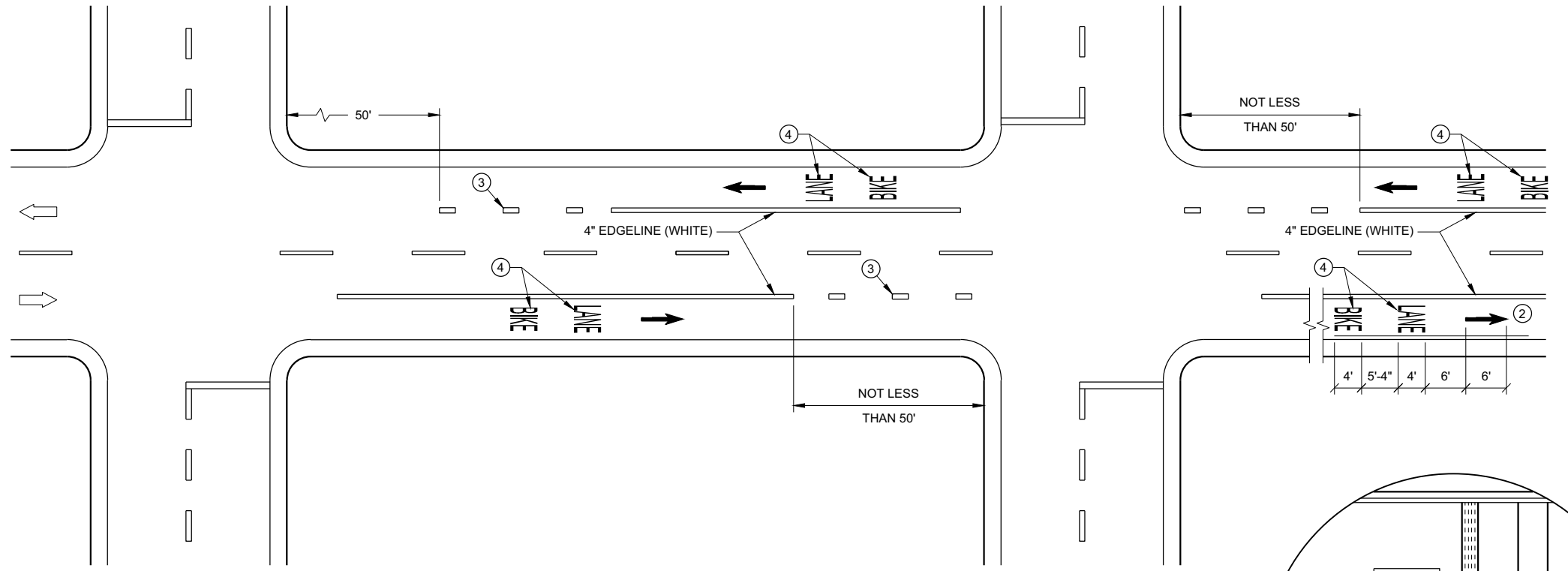


TYPE III BARRICADE

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.

* IF USED FOR A PERMANENT APPLICATION USE RED SHEETING.

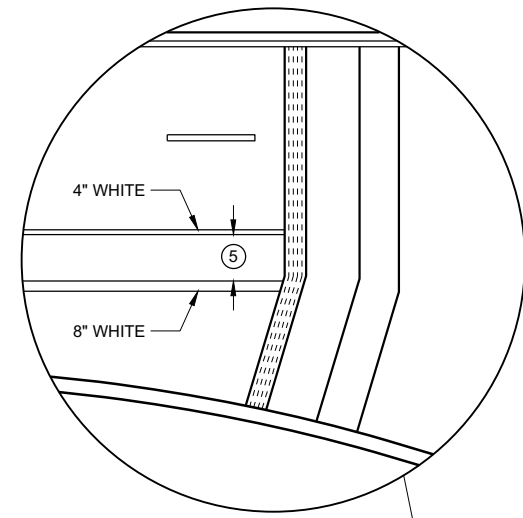
CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2021 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
<small>FHWA</small>	



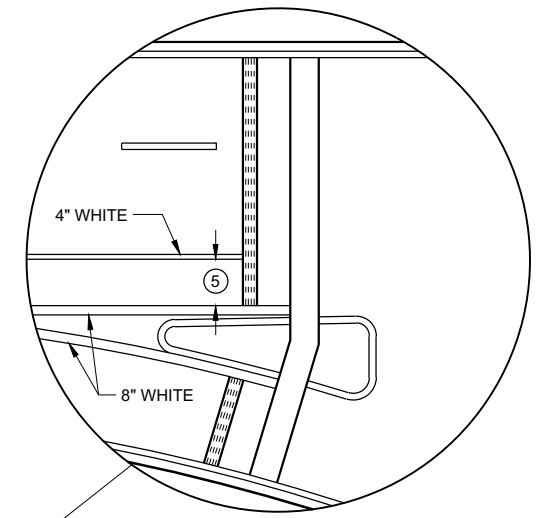
DESIGNATED BIKE LANE - NO PARKING

- GENERAL NOTES**
- ① DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.
 - ② MINIMUM OF ONE PER BLOCK. MAXIMUM OF 250 FEET.
 - ③ DOTTED LINES (3' LINE, 9' GAP) SHOULD BE USED 50 FEET TO 200 FEET IN ADVANCE OF AN INTERSECTION WHERE THERE IS NO RIGHT TURN ONLY LANE AND THERE IS HEAVY RIGHT TURN TRAFFIC OR THERE IS A NEAR-SIDE BUS STOP. AT OTHER INTERSECTIONS WHERE RIGHT TURN TRAFFIC IS LIGHT TO MODERATE, A SOLID LINE CAN BE USED UP TO THE INTERSECTION.
 - ④ BIKE SYMBOLS OR WORDS MAY BE USED.
 - ⑤ BIKE ACCOMMODATION IS TYPICAL 5 FEET WIDE AND MINIMUM OF 4 FEET FROM A LONGITUDINAL JOINT. USE 5 FEET AT ≥ 45 MPH.
 - ⑥ OMIT THESE MARKINGS FOR WIDER TURN LANE APPLICATIONS (MINIMUM OF 15 FOOT WIDE TURN LANE).
 - ⑦ REFER TO CONTRACT PLANS FOR LANE WIDTH.

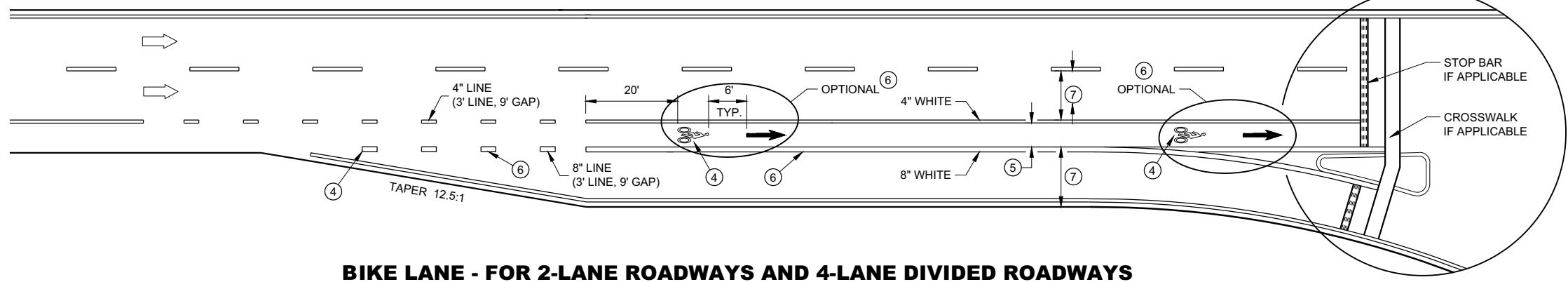
➔ DIRECTION OF TRAVEL



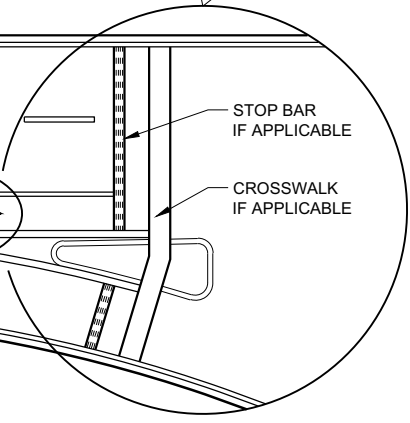
4 LANE DIVIDED WITHOUT ISLAND



4 LANE DIVIDED WITH ISLAND



**BIKE LANE - FOR 2-LANE ROADWAYS AND 4-LANE DIVIDED ROADWAYS
(4-LANE DIVIDED WITH RIGHT TURN LANE SHOWN)**



BIKE LANE MARKING	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2020 DATE	/S/ Matthew Rauch STATE SIGNING AND MARKING ENGINEER
<small>FHWA</small>	

6

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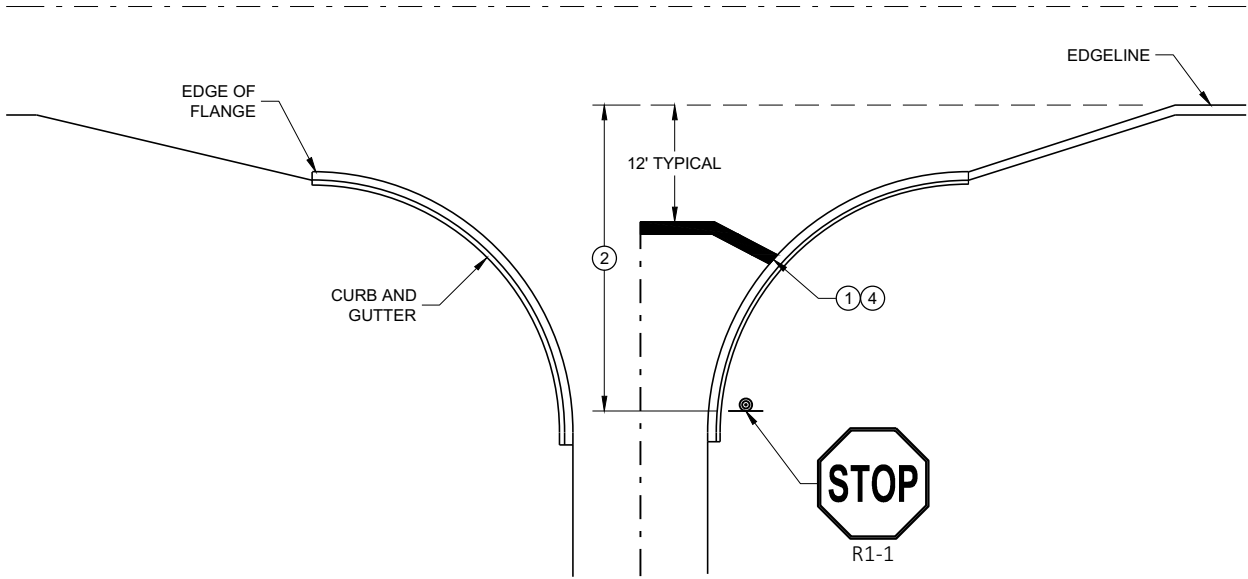
SDD 15C29 - 07a

SDD 15C29 - 07a

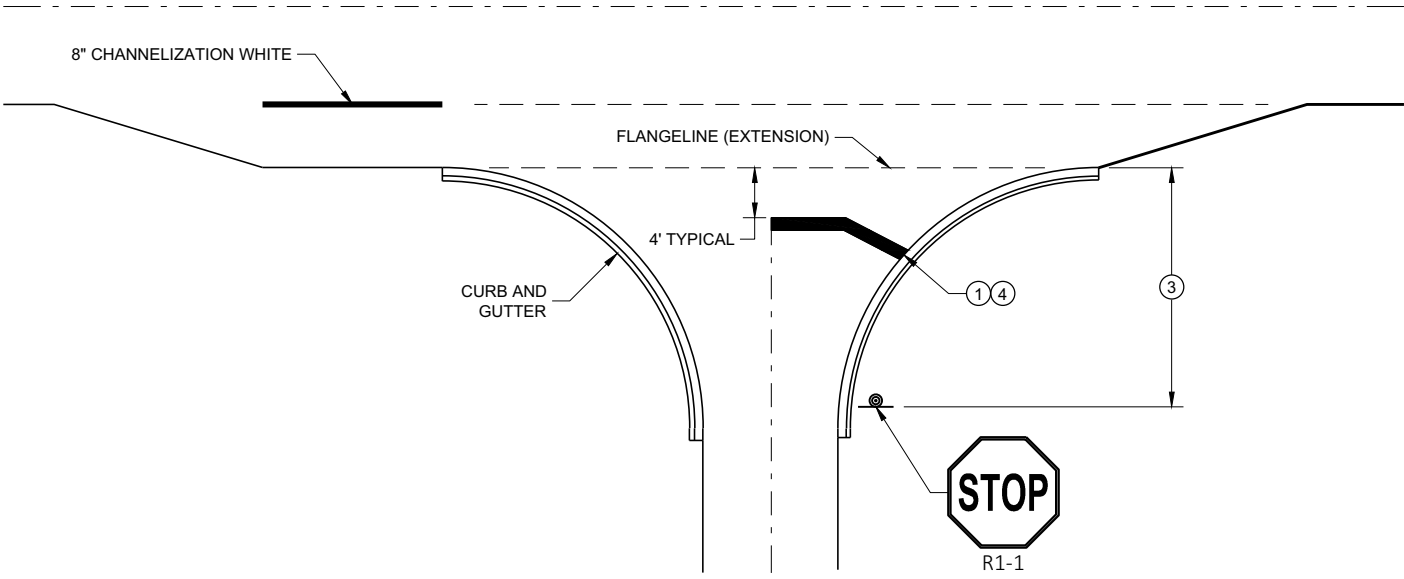
GENERAL NOTES

STOP SIGN SHALL BE PLACED A MINIMUM OF 6 FEET TO A MAXIMUM OF 50 FEET FROM THE EDGELINE LOCATION.

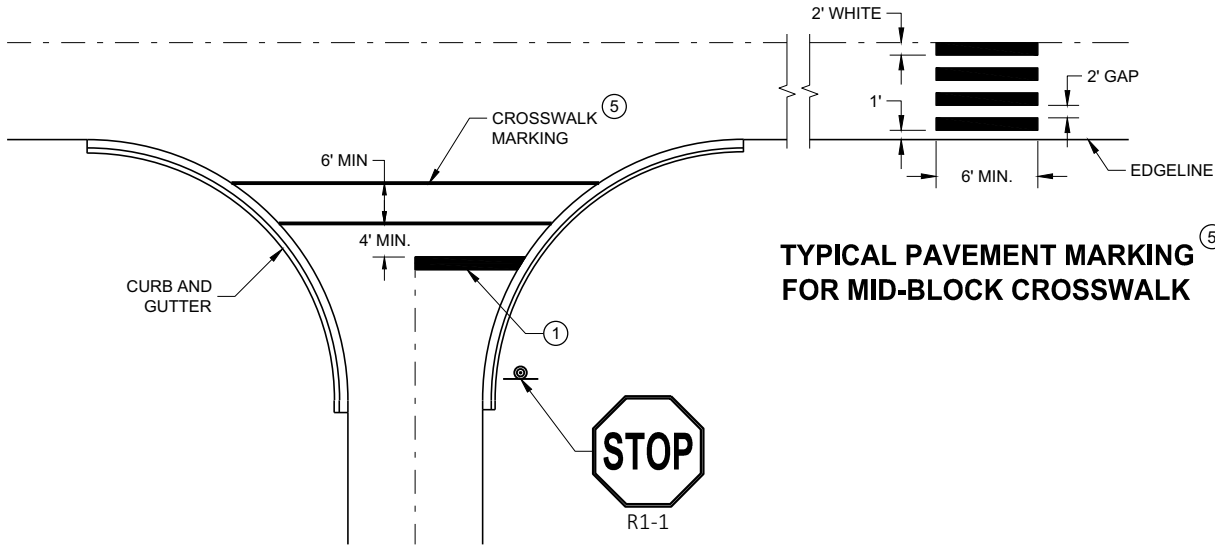
- ① 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE REGION MARKING ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
- ② NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGELINE.
- ③ NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGELINE EXTENSION.
- ④ MOVE CLOSER TO THE EDGE OF TRAVEL LINE AS NEEDED FOR VISIBILITY AND SIGHT LINES (NO CLOSER THAN 4 FEET).
- ⑤ LADDER BAR CROSSWALKS SHOULD ONLY BE USED FOR MID BLOCK CROSSINGS. USE 2 - 6" TRANSVERSE LINES INSTEAD.



TYPICAL STOP LINE PAVEMENT MARKING WITH CURB AND GUTTER

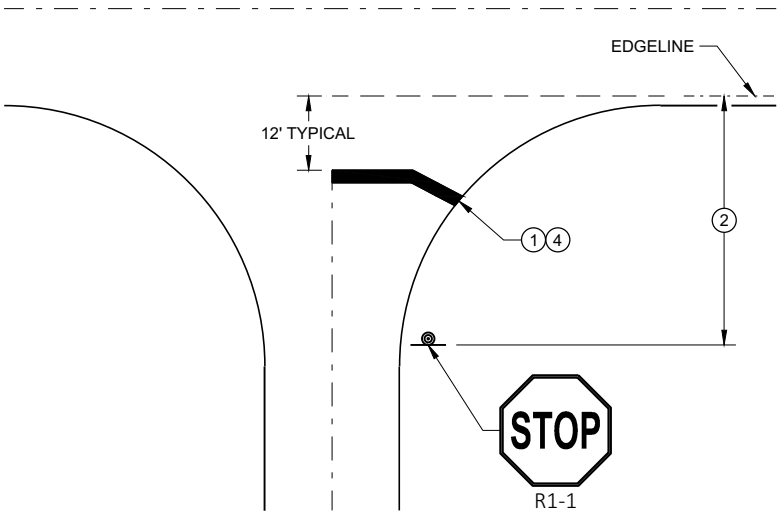


TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH RIGHT TURN LANE



TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH CROSSWALK MARKING

TYPICAL PAVEMENT MARKING FOR MID-BLOCK CROSSWALK



TYPICAL STOP LINE PAVEMENT MARKING WITHOUT CURB AND GUTTER

STOP LINE AND CROSSWALK PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2019 /S/ Matthew Rauch
DATE STATE SIGNING AND MARKING ENGINEER

FHWA

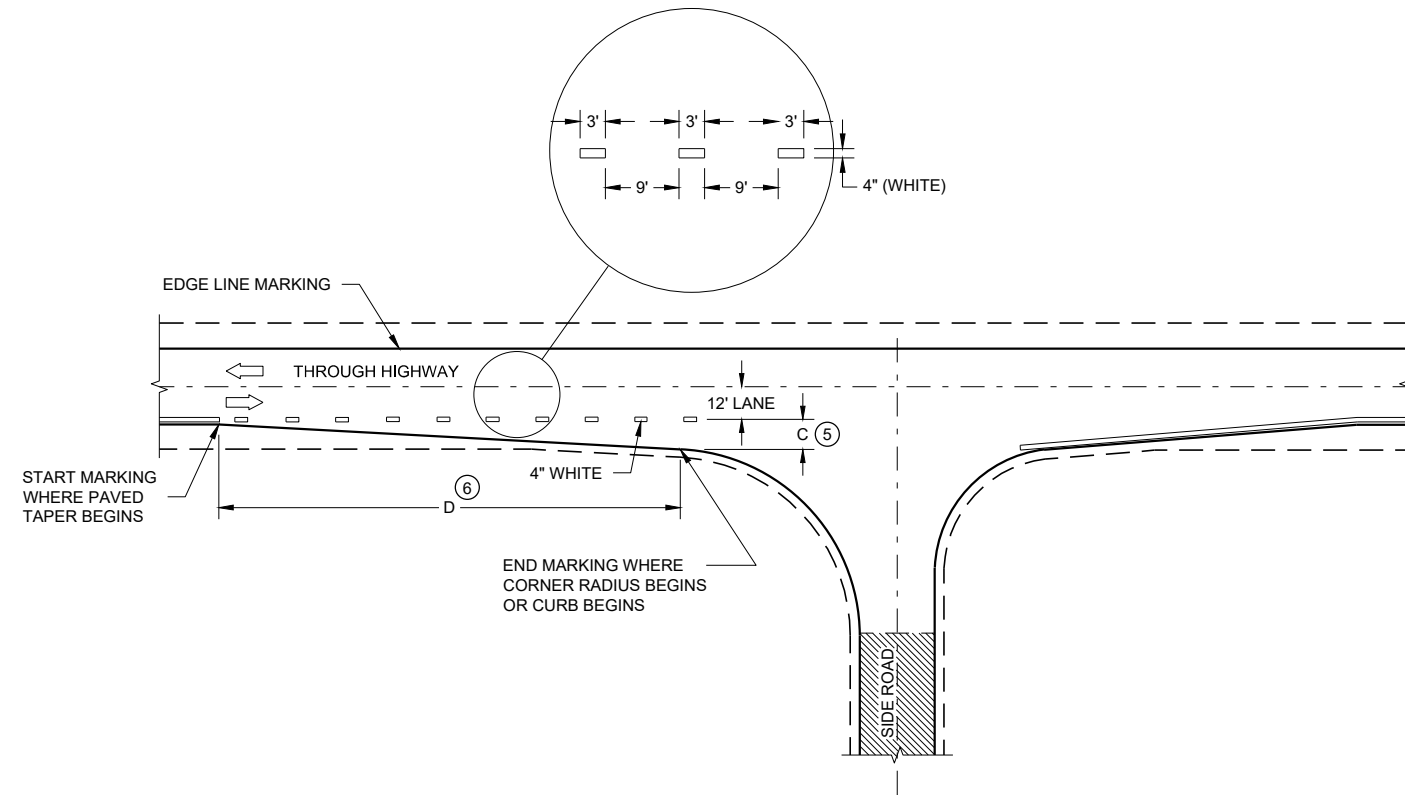
GENERAL NOTES

OMIT EDGE LINES THROUGH INTERSECTIONS. CONTINUE EDGE LINES THROUGH DRIVEWAYS.

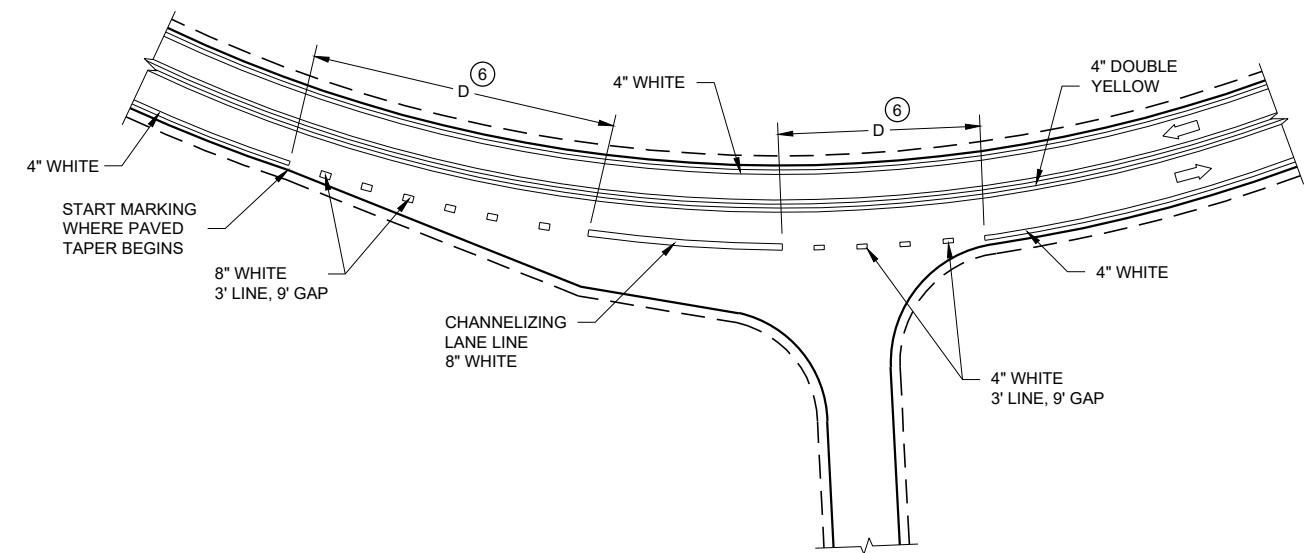
- ① WHEN DISTANCE "A" IS LESS THAN 250 FEET, OMIT LANE LINE.
- ② WHEN DISTANCE "B" IS LESS THAN 100 FEET, OMIT CHANNELIZING LANE LINE.
- ③ BARRIER LINE ENDS AT SIDE ROAD PAVEMENT / SURFACE EDGE EXTENSION.
- ④ BARRIER LINE STARTS 500 FEET PRIOR TO THE BYPASS TAPER.
- ⑤ WHEN DISTANCE "C" IS LESS THAN 4 FEET, OMIT DOTTED EXTENSION.
- ⑥ WHEN DISTANCE "D" IS LESS THAN 50 FEET, OMIT DOTTED EXTENSION.

LEGEND

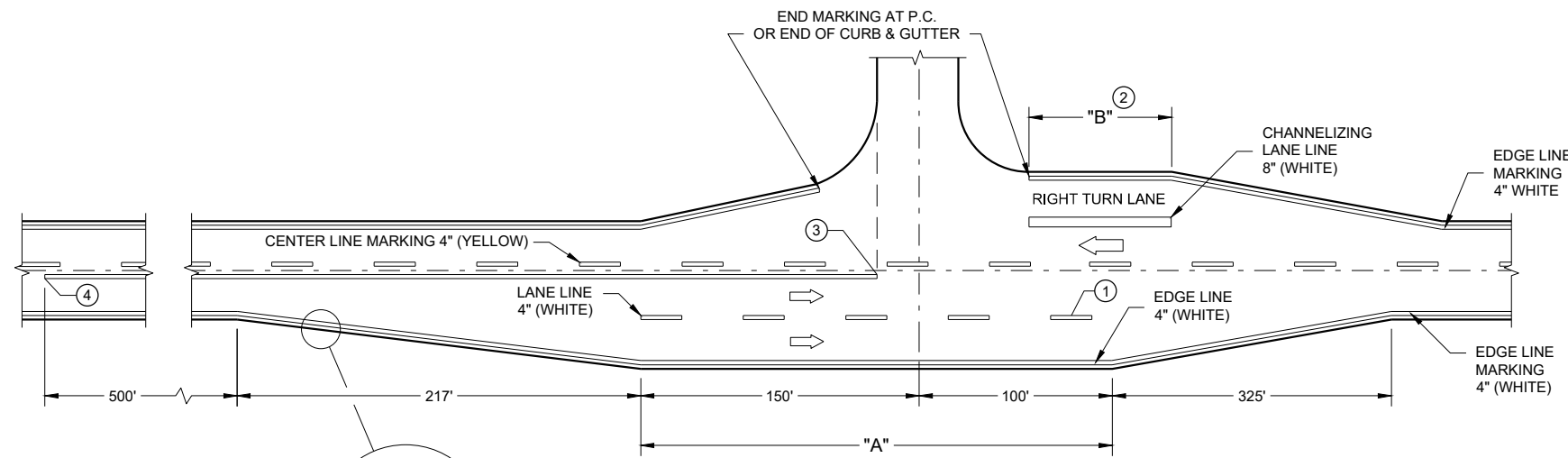
➡ DIRECTION OF TRAVEL



MINOR INTERSECTION

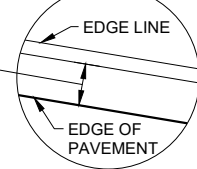


INTERSECTION ON OUTSIDE OF CURVE



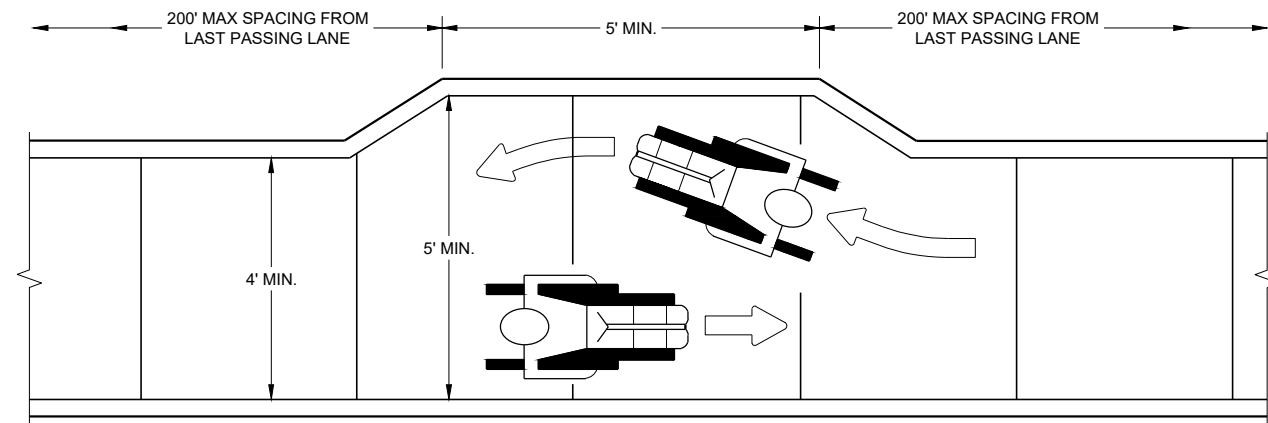
**MAJOR INTERSECTIONS
(INTERSECTION WITH FULL RIGHT TURN LANE OR BYPASS LANE)**

BYPASS LANE PAVED SHOULDER WIDTH (AS SHOWN ELSEWHERE IN PLANS) - PLUS 2 INCHES

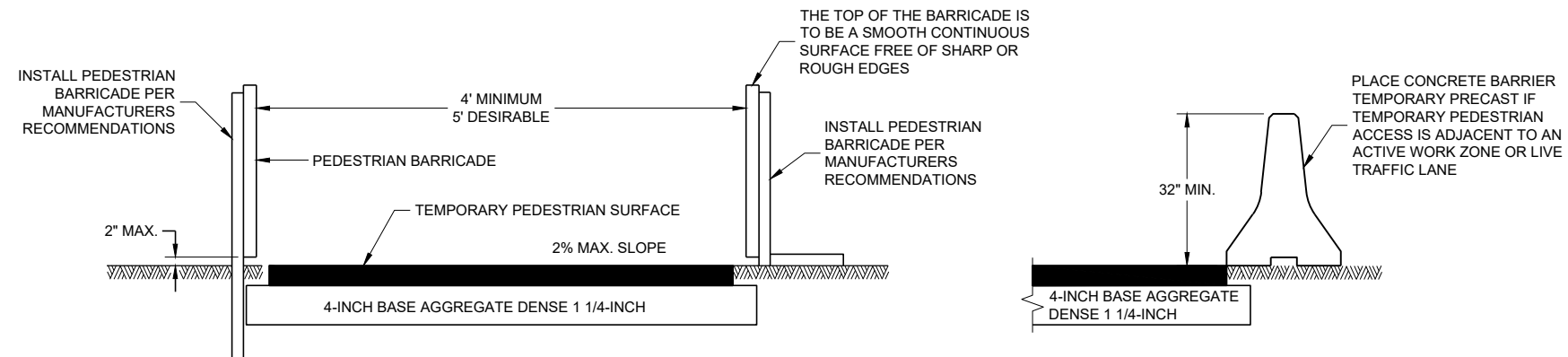


**PAVEMENT MARKING
(INTERSECTIONS)**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



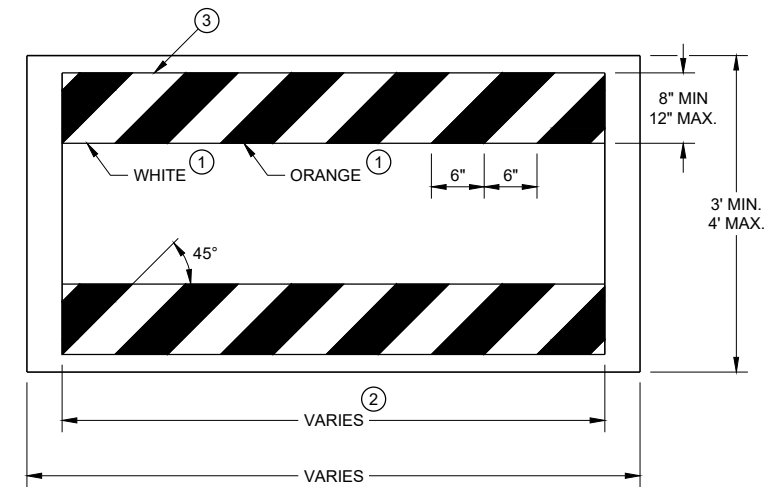
NARROW SIDEWALK PASSING DETAIL



TEMPORARY PEDESTRIAN ACCESS

GENERAL NOTES

- BARRICADE DEVICE SELECTED FROM APPROVED PRODUCT LIST
- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② SHEETING REQUIRED ON MORE THAN 50% OF BARRICADE WIDTH.
- ③ PLACE SHEETING ON BOTH SIDES OF THE BARRICADE.
- * USE THIS DETAIL FOR SHEETING PLACEMENT REFERENCE.



TEMPORARY PEDESTRIAN BARRICADE*

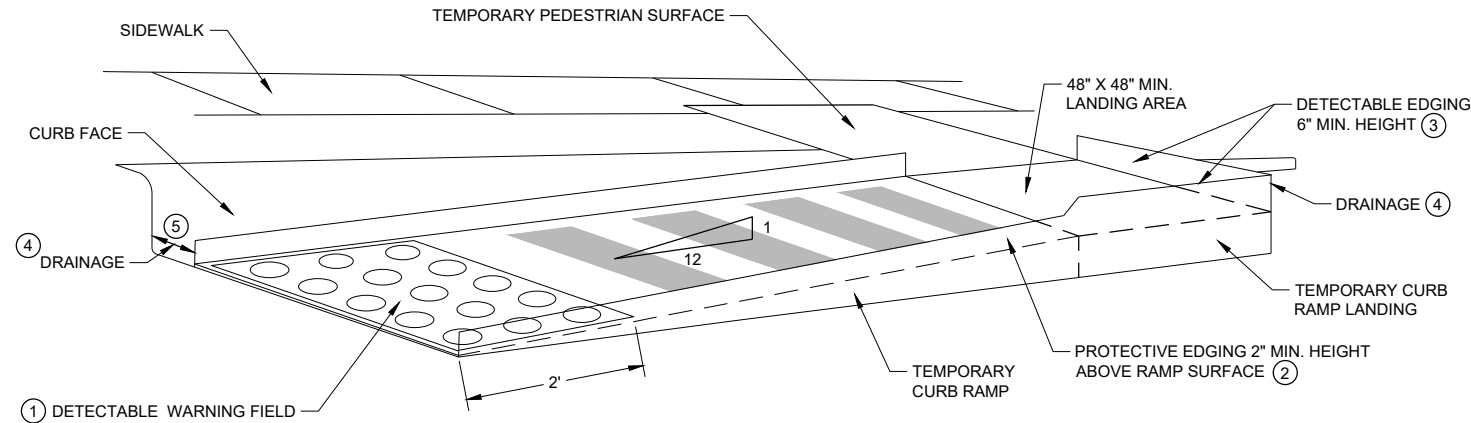
**TRAFFIC CONTROL,
PEDESTRIAN
ACCOMMODATION**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

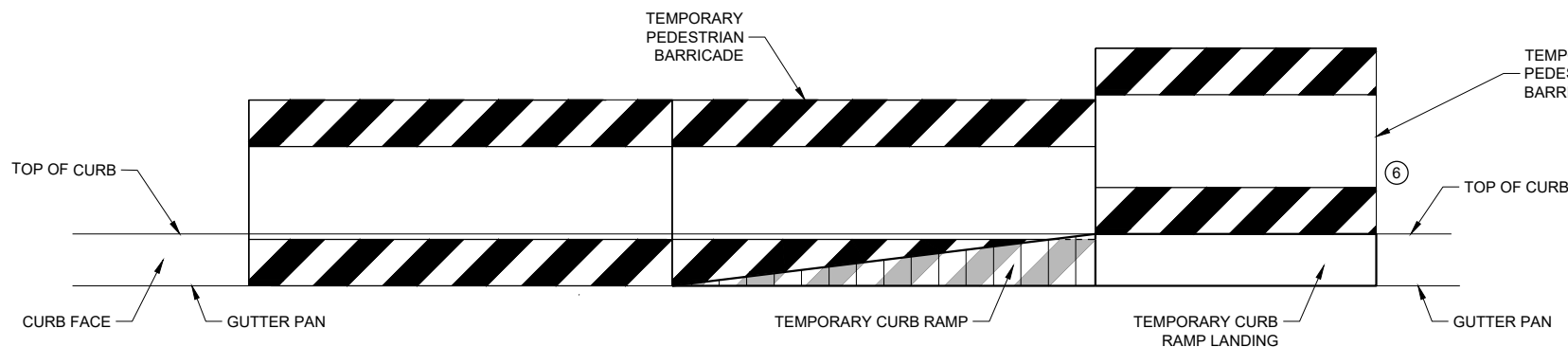
GENERAL NOTES

CURB RAMPS SHALL BE 48" MIN. WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE.
 CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.
 CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.
 LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.
 CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES MAY BE VERTICAL UP TO 1/4" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".

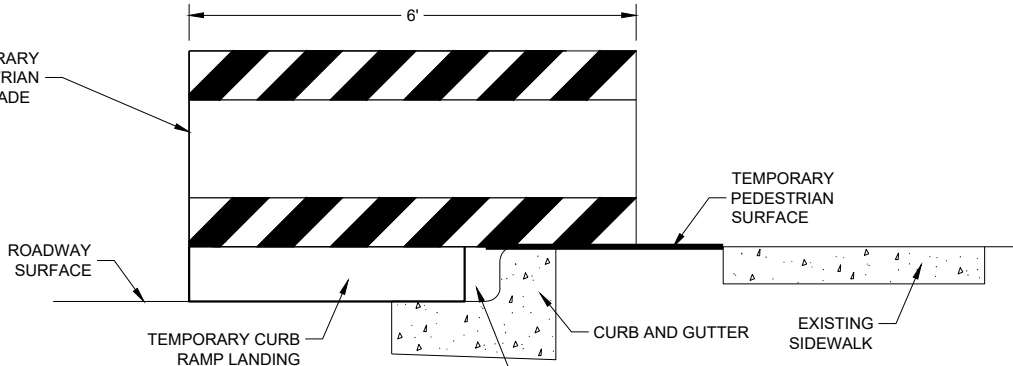
- ① INSTALL CONTRASTING TEMPORARY DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS, AS SHOWN IN THE PLANS.
- ② PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- ③ DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- ④ DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ⑤ 6" MINIMUM BETWEEN CURB FACE AND EDGE OF RAMP
- ⑥ IF ONLY PART OF THE END PANEL OF TEMPORARY PEDESTRIAN BARRICADE PANEL IS NEEDED, EXTEND EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL HERE.



PERSPECTIVE VIEW



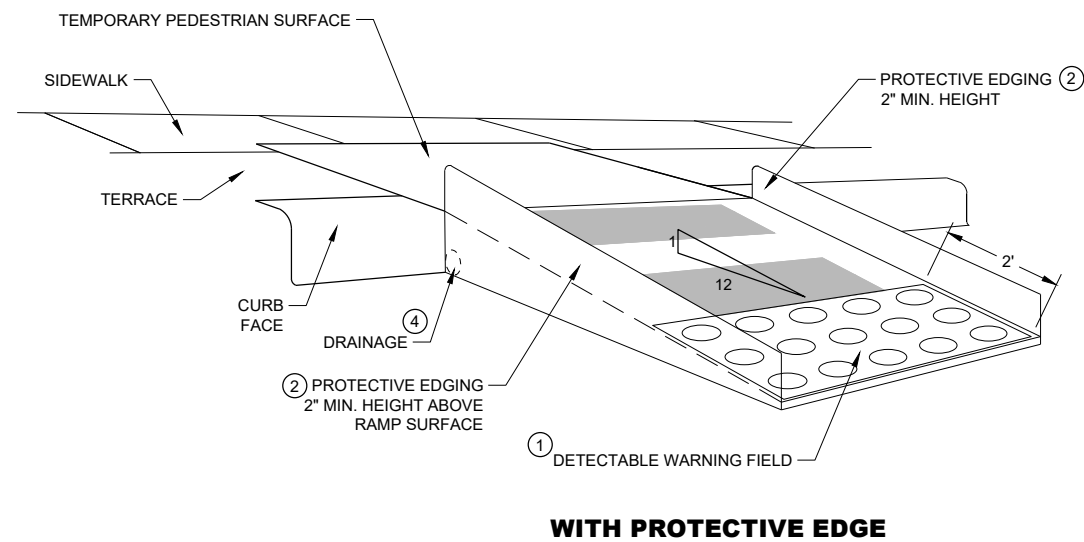
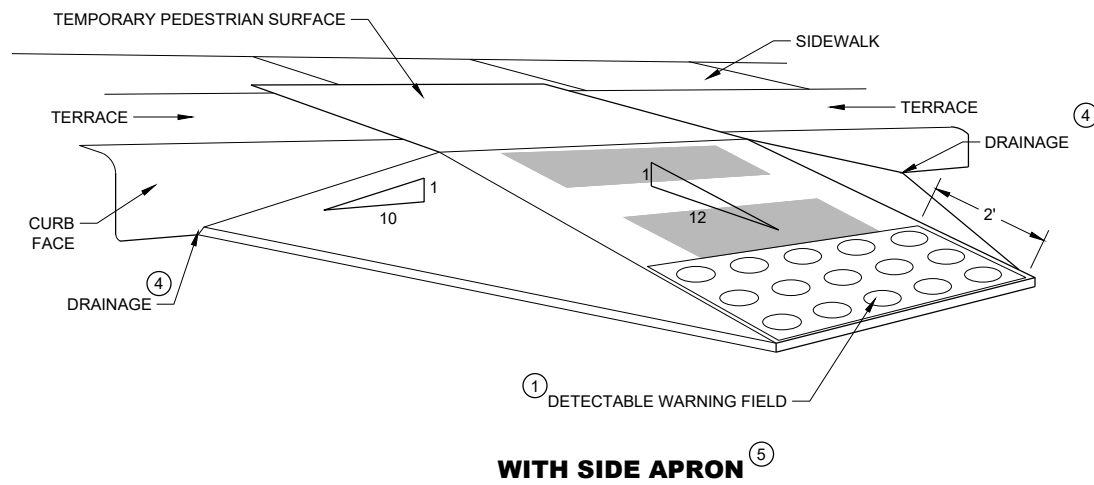
FRONT VIEW



SIDE VIEW

TEMPORARY CURB RAMP PARALLEL TO CURB

<p>TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION</p>
<p>STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION</p>



TEMPORARY CURB RAMP PERPENDICULAR TO CURB

GENERAL NOTES

CURB RAMPS SHALL BE 48" MINIMUM WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE.

ALTERNATE SIDEWALK WORK BETWEEN LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.

CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.

CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.

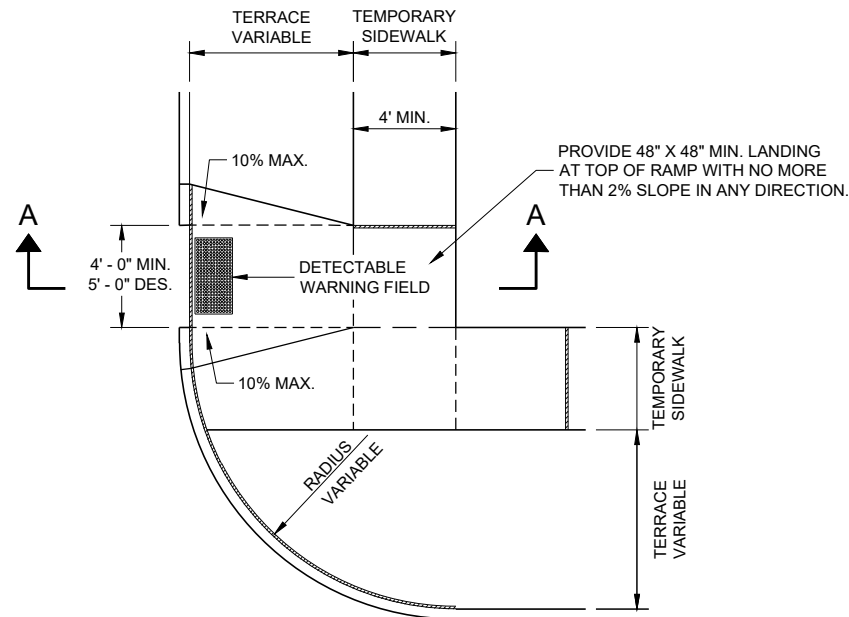
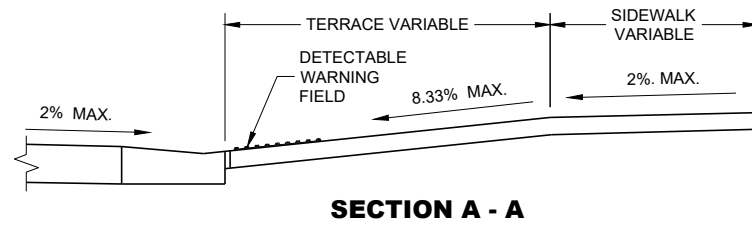
LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.

CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES MAY BE VERTICAL UP TO 1/4" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".

- ① INSTALL CONTRASTING TEMPORARY DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS, AS SHOWN IN THE PLANS
- ② PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- ③ DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- ④ DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ⑤ CAN ONLY BE USED FOR RAMPS WITH 6" OR LESS OF VERTICAL CHANGE.

GENERAL NOTES

- BARRICADE DEVICE SELECTED FROM APPROVED PRODUCT LIST
- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② SHEETING REQUIRED ON MORE THAN 50% OF BARRICADE WIDTH.
- ③ PLACE SHEETING ON BOTH SIDES OF THE BARRICADE.
- ★ USE THIS DETAIL FOR SHEETING PLACEMENT REFERENCE.



PLAN VIEW
TEMPORARY TYPE 3 RAMP
 (OUTSIDE OF CROSSWALK AREA)

**TRAFFIC CONTROL,
 PEDESTRIAN ACCOMMODATION**

STATE OF WISCONSIN
 DEPARTMENT OF TRANSPORTATION

APPROVED
 May 2022 /S/ Andrew Heidtke
 DATE WORK ZONE ENGINEER

FHWA

GENERAL NOTES

TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.

NOTIFY THE BUS COMPANY 7 DAYS IN ADVANCE OF THE BUS STOP RELOCATION.

PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMP OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.

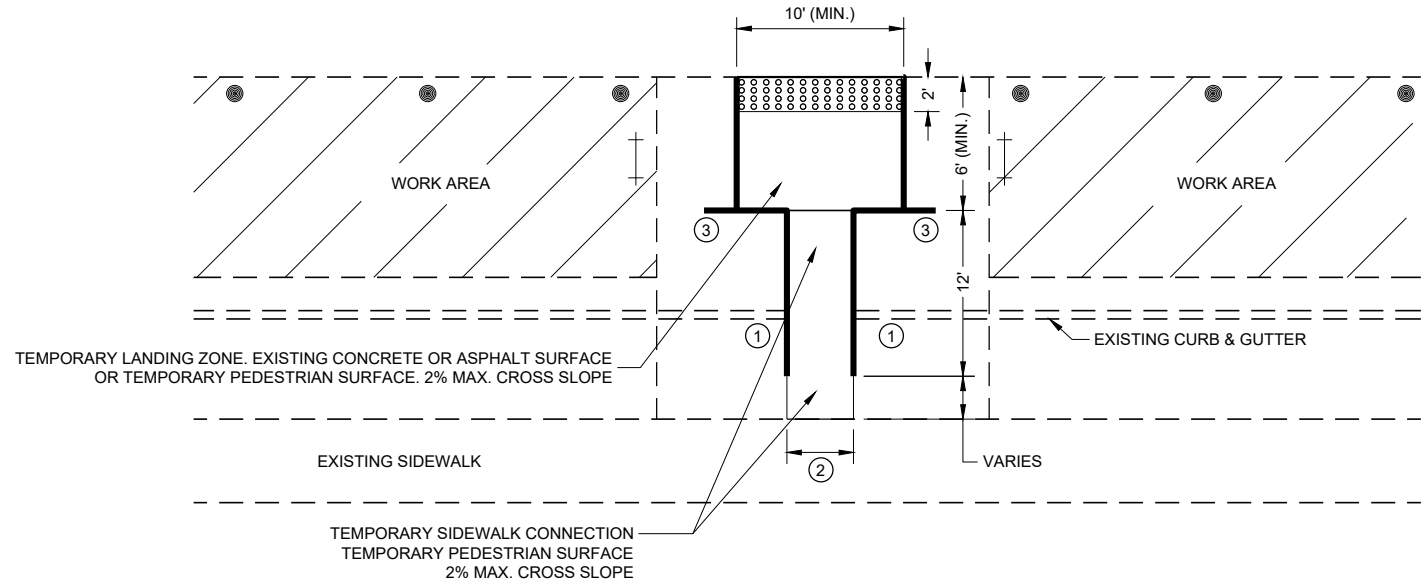
DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).

LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.

CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES MAY BE VERTICAL UP TO 1/4" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".

CURB RAMP AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.

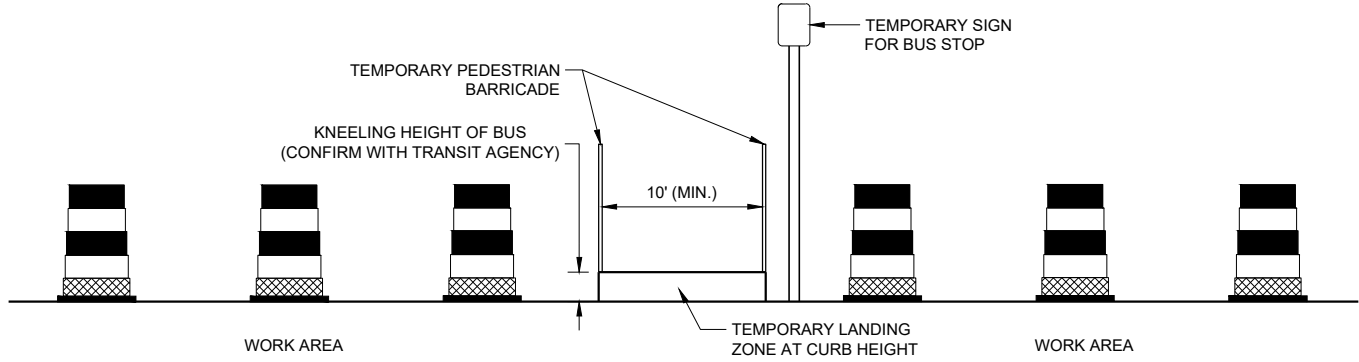
- ① DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ② 5' WIDE MIN. WITH TEMPORARY PEDESTRIAN BARRICADE, 10' WIDE MIN. WITHOUT TEMPORARY PEDESTRIAN BARRICADE.
- ③ PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE INTO THIS SPACE.



PLAN VIEW

LEGEND

- TRAFFIC CONTROL DRUM
- TYPE III BARRICADE
- TEMPORARY PEDESTRIAN BARRICADE
- TEMPORARY DETECTABLE WARNING FIELD
- WORK AREA









**PROFILE VIEW
TEMPORARY BUS STOP PAD**

**TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION**

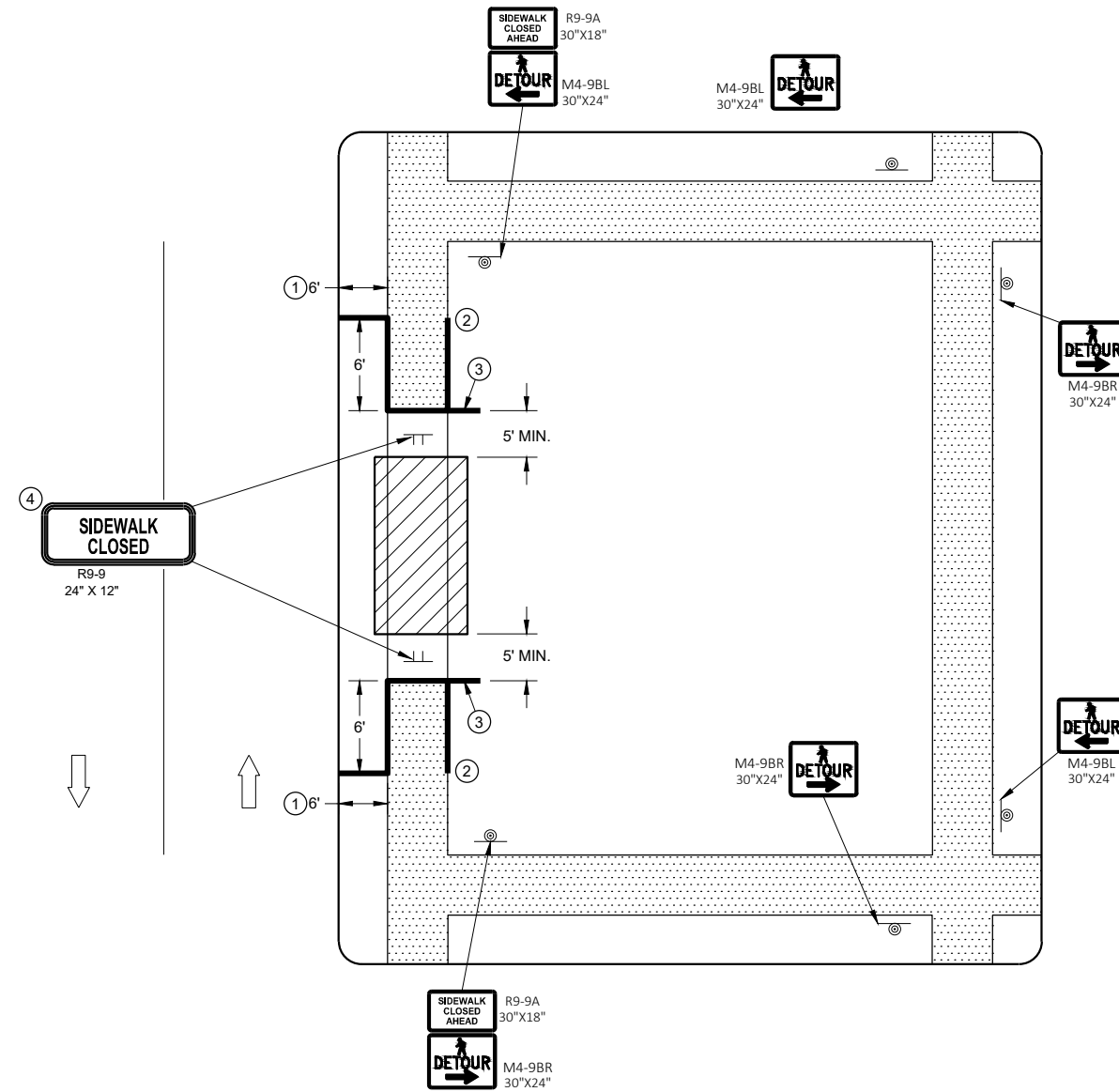
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  SIGN ON TEMPORARY SUPPORT
-  UNDER PEDESTRIAN TRAFFIC
-  WORK AREA
-  TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC

GENERAL NOTES








- SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.
- WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.
- SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.
- ① IF TERRACE IS LESS THAN 6 FEET WIDE, OMIT TEMPORARY PEDESTRIAN BARRICADE FROM THE SIDEWALK TO THE CURB.
 - ② PLACE BARRICADE CLOSURE SO THAT THE TEMPORARY PEDESTRIAN BARRICADE END IS AT THE LAST OPEN SIDEWALK ACCESS TO RESIDENCES OR BUSINESSES BEFORE THE SIDEWALK CLOSURE.
 - ③ IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
 - ④ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.



SIDEWALK DETOUR, SIDEWALK ONLY ON ONE SIDE

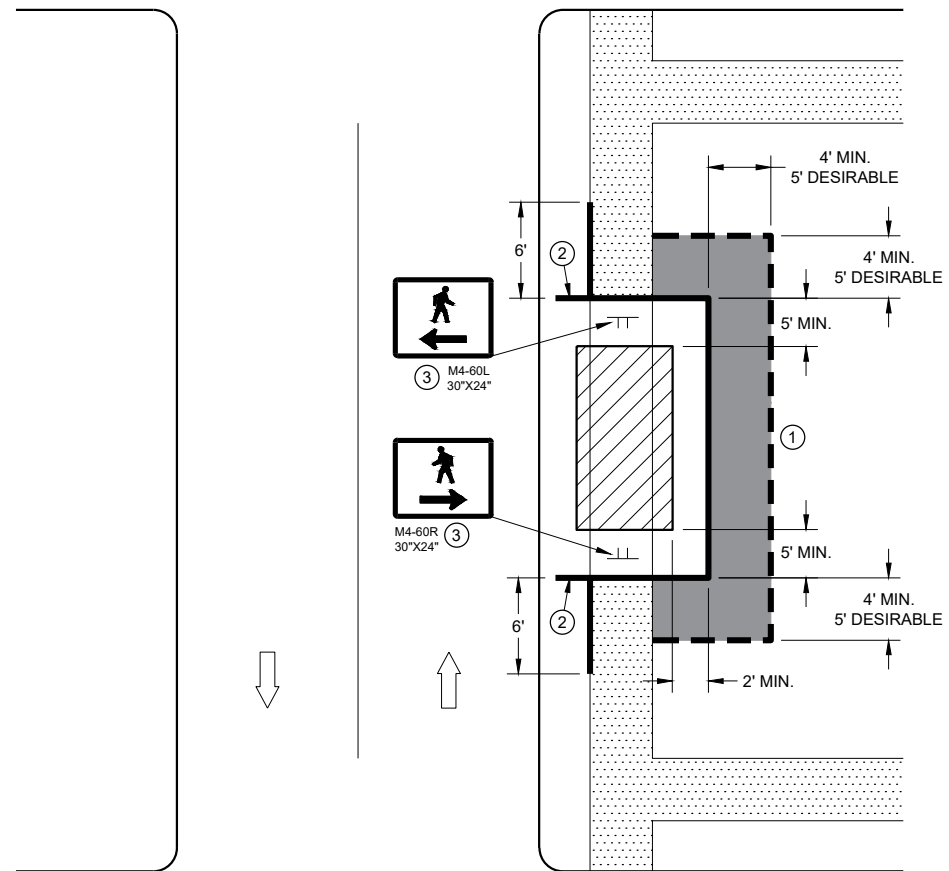
TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  WORK AREA
-  UNDER PEDESTRIAN TRAFFIC
-  TEMPORARY PEDESTRIAN SURFACE
-  TEMPORARY PEDESTRIAN BARRICADE
-  OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC










GENERAL NOTES

- TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.
- SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.
- WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.
- SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.
- ① USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
- ② IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
- ③ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.



**SIDEWALK DIVERSION
SINGLE SIDE**

LEGEND

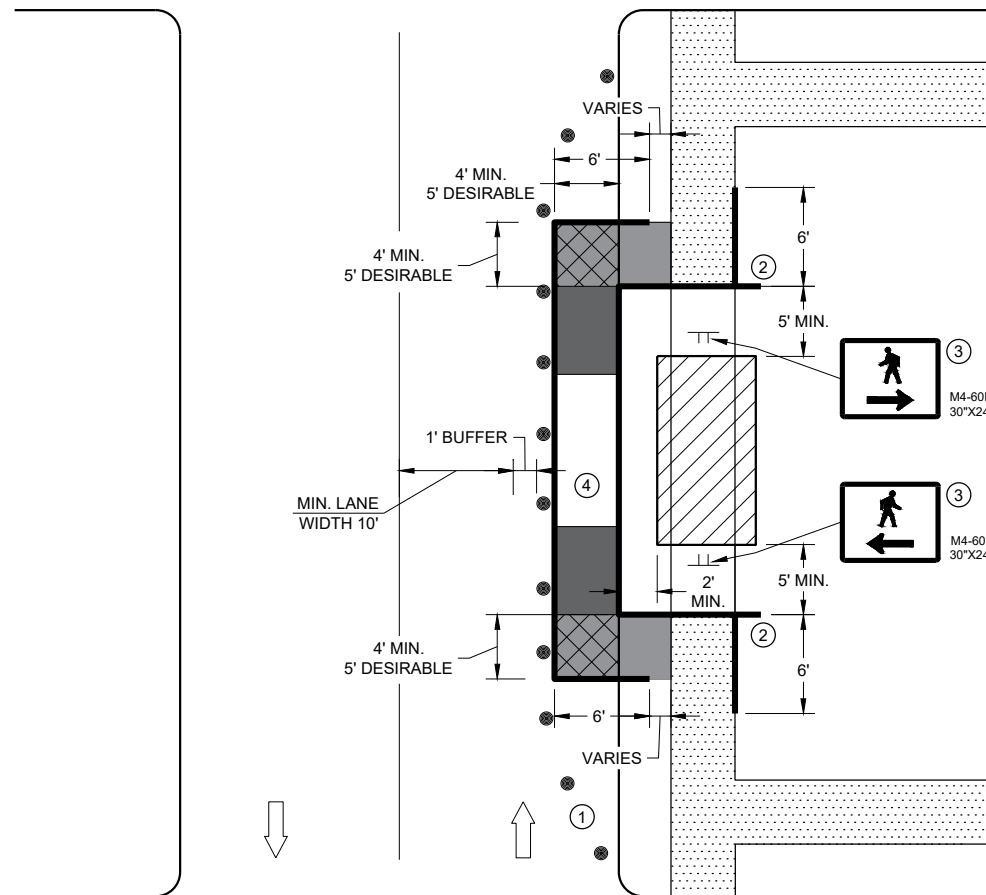
-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL DRUM
-  WORK AREA
-  UNDER PEDESTRIAN TRAFFIC
-  TEMPORARY CURB RAMP
-  TEMPORARY PEDESTRIAN SURFACE "A"
-  TEMPORARY PEDESTRIAN SURFACE "B"
-  TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC

GENERAL NOTES

- TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.
- SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.
- WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.
- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND BUFFER SPACE REQUIRED.
 - ② PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL PAST THE SIDEWALK ON THE SIDE AWAY FROM THE ROAD.
 - ③ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.
 - ④ USE EXISTING PAVEMENT SURFACE. IF EXISTING PAVEMENT SURFACE HAS BEEN REMOVED, USE A TEMPORARY PEDESTRIAN SURFACE.

6

SDD 15D30 - 07h



6

SDD 15D30 - 07h

SIDEWALK DIVERSION, SINGLE SIDE

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

IF PEDESTRIAN PUSH BUTTONS ARE PRESENT ON THE EXISTING FACILITY, ENSURE THEY ARE MAINTAINED/ACCESSIBLE FOR PEDESTRIAN USE THROUGHOUT THE TEMPORARY PEDESTRIAN ACCOMMODATIONS.











SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

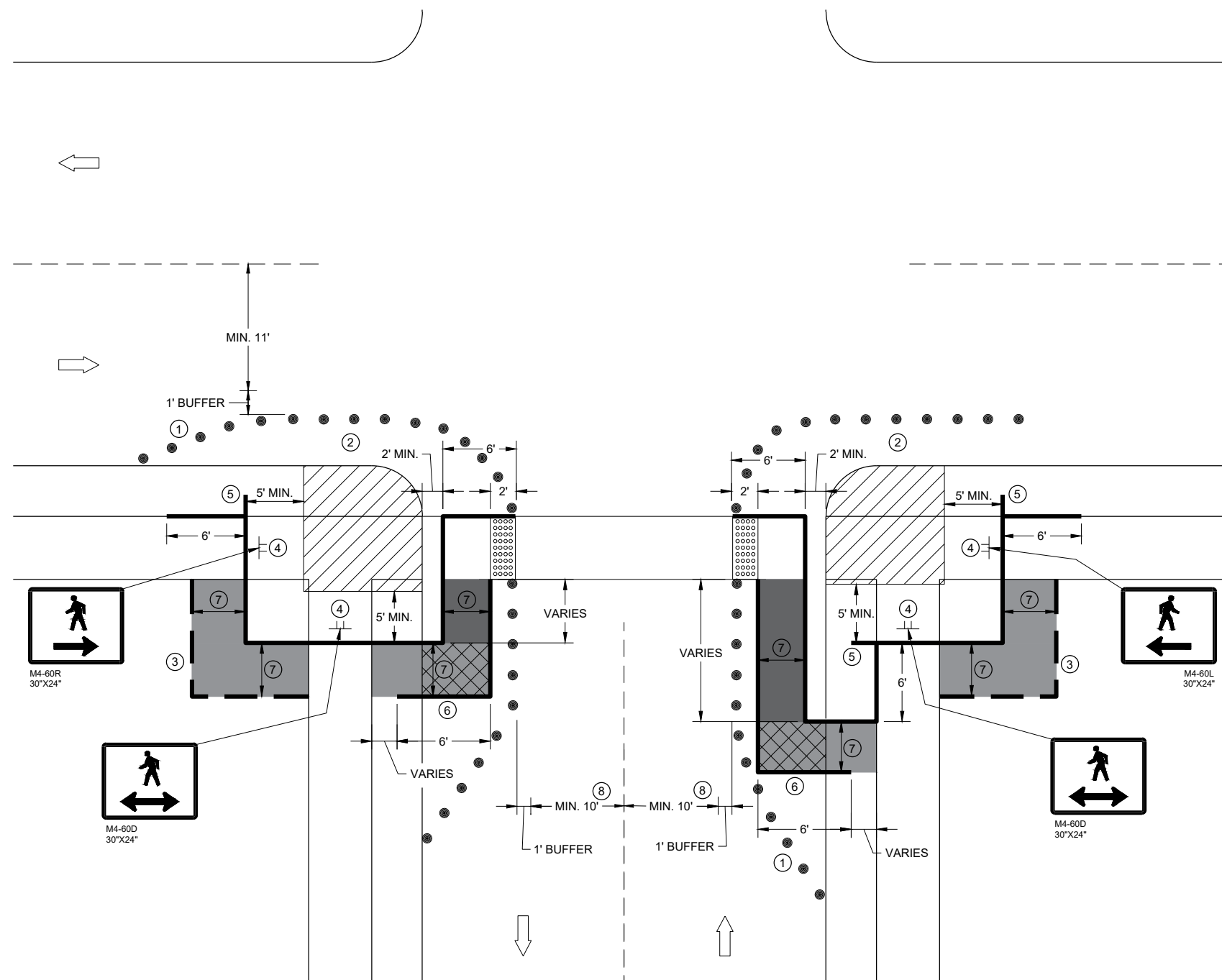
TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG

WHEN TEMPORARY PEDESTRIAN BARRICADE RUNS PARALLEL ALONG THE SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND PROPER BUFFER SPACE REQUIRED.
- ② PROVIDE ADEQUATE SPACE FOR CONTRACTOR OPERATIONS
- ③ USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
- ④ MOUNTING HEIGHT OF 5 FEET FROM SIDEWALK SURFACE TO BOTTOM OF SIGN.
- ⑤ PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL IN THE SIDEWALK TERRACE.
- ⑥ IF TEMPORARY PEDESTRIAN BARRICADE DOES NOT REACH THE FACE OF THE CURB, USE AN ADDITIONAL PANEL AND EXTEND INTO THE TERRACE.
- ⑦ 4 FEET MINIMUM, 5 FEET DESIRABLE
- ⑧ IF MINIMUM LANE WIDTHS CAN'T BE ATTAINED, CURB RAMPS MAY NEED TO BE CONSTRUCTED AT SEPARATE TIMES.

LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL DRUM
-  WORK AREA
-  TEMPORARY CURB RAMP
-  TEMPORARY PEDESTRIAN SURFACE "A"
-  TEMPORARY PEDESTRIAN SURFACE "B"
-  TEMPORARY DETECTABLE WARNING FIELD
-  TEMPORARY PEDESTRIAN BARRICADE
-  OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC



**CURB RAMP PEDESTRIAN TRAFFIC CONTROL
SIDEWALK ON SINGLE SIDE**

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

6

6

SDD 15D30 - 07i

SDD 15D30 - 07i

GENERAL NOTES

IF PEDESTRIAN PUSH BUTTONS ARE PRESENT ON THE EXISTING FACILITY, ENSURE THEY ARE MAINTAINED/ACCESSIBLE FOR PEDESTRIAN USE THROUGHOUT THE TEMPORARY PEDESTRIAN ACCOMMODATIONS.











SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

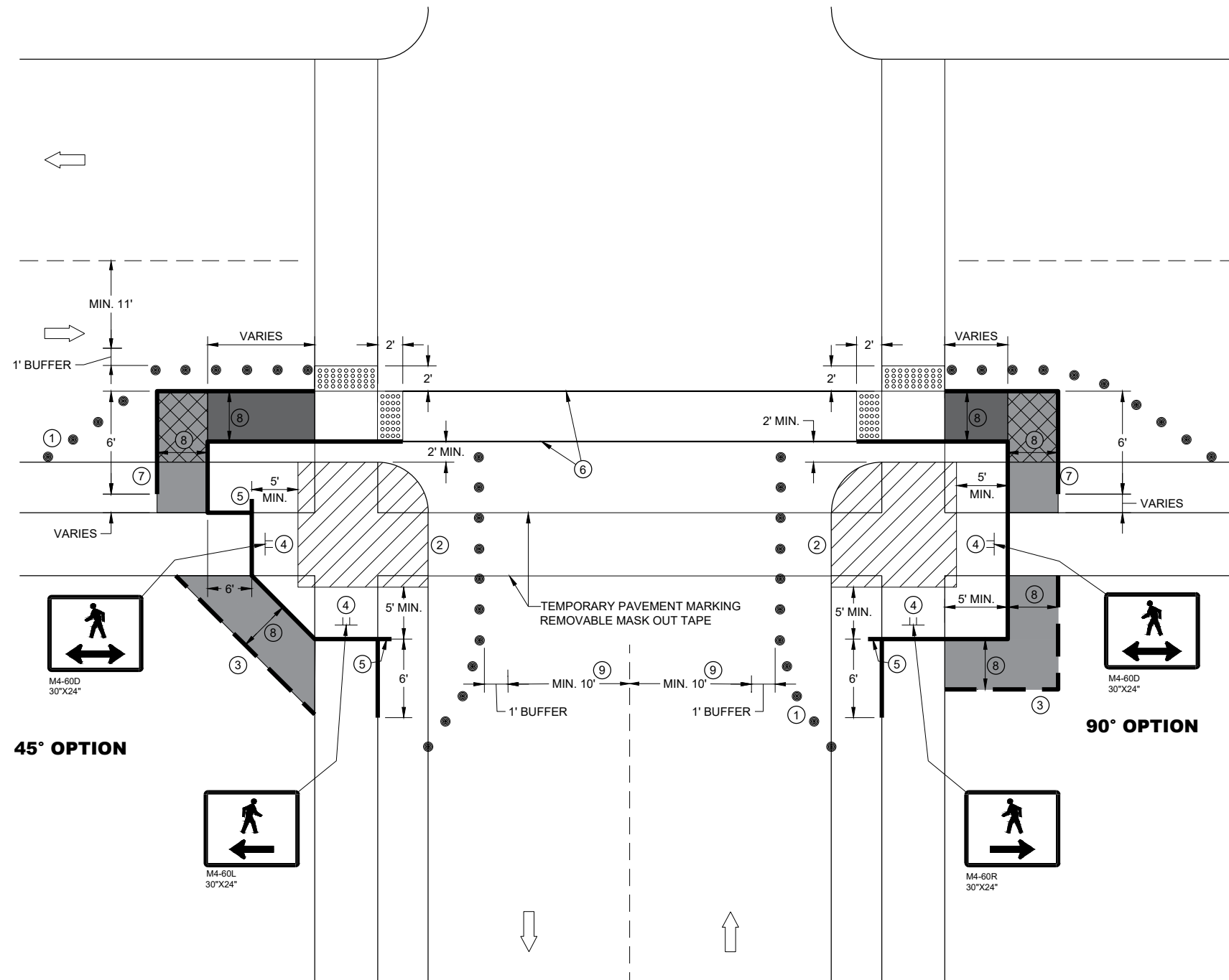
TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG

WHEN TEMPORARY PEDESTRIAN BARRICADE RUNS PARALLEL ALONG THE SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND PROPER BUFFER SPACE REQUIRED.
- ② PROVIDE ADEQUATE SPACE FOR CONTRACTOR OPERATIONS
- ③ USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
- ④ MOUNTING HEIGHT OF 5 FEET FROM SIDEWALK SURFACE TO BOTTOM OF SIGN.
- ⑤ PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL IN THE SIDEWALK TERRACE.
- ⑥ WHITE 6" TEMPORARY PAVEMENT MARKING
- ⑦ IF TEMPORARY PEDESTRIAN BARRICADE DOES NOT REACH THE FACE OF THE CURB, USE AN ADDITIONAL PANEL AND EXTEND INTO THE TERRACE.
- ⑧ 4 FEET MINIMUM, 5 FEET DESIRABLE
- ⑨ IF MINIMUM LANE WIDTHS CAN'T BE ATTAINED, CURB RAMPS MAY NEED TO BE CONSTRUCTED AT SEPARATE TIMES.

LEGEND

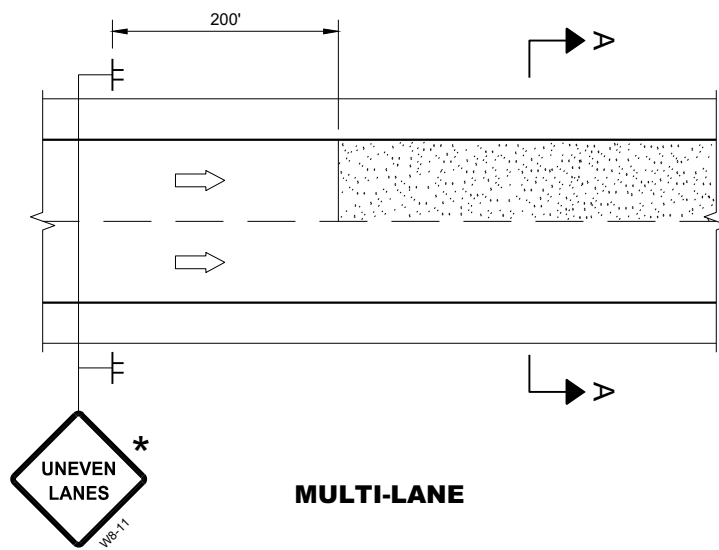
-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL DRUM
-  WORK AREA
-  TEMPORARY CURB RAMP
-  TEMPORARY PEDESTRIAN SURFACE "A"
-  TEMPORARY PEDESTRIAN SURFACE "B"
-  TEMPORARY DETECTABLE WARNING FIELD
-  TEMPORARY PEDESTRIAN BARRICADE
-  OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC



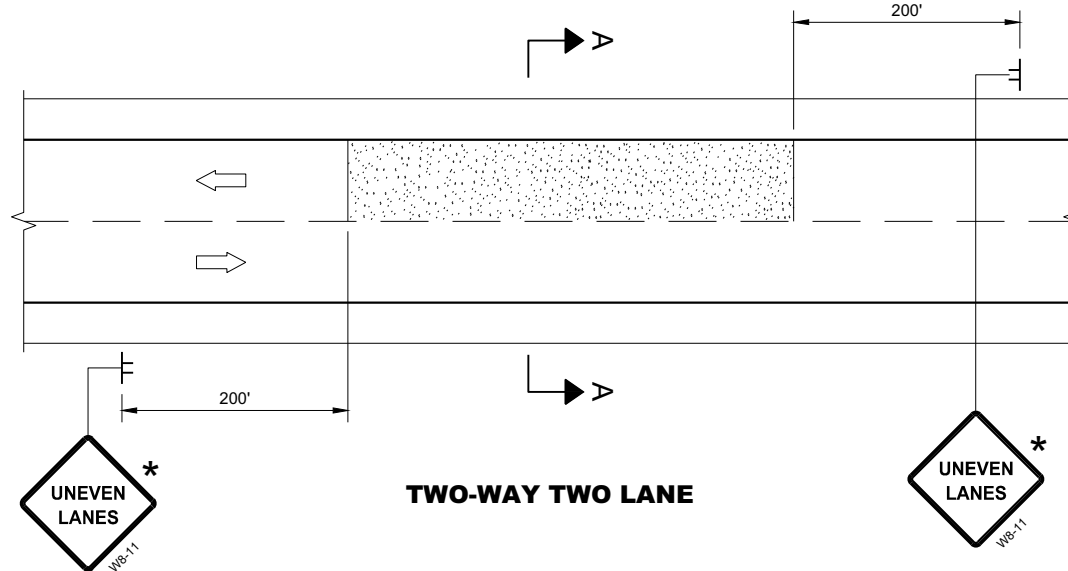
CURB RAMP PEDESTRIAN TRAFFIC CONTROL

**TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION**

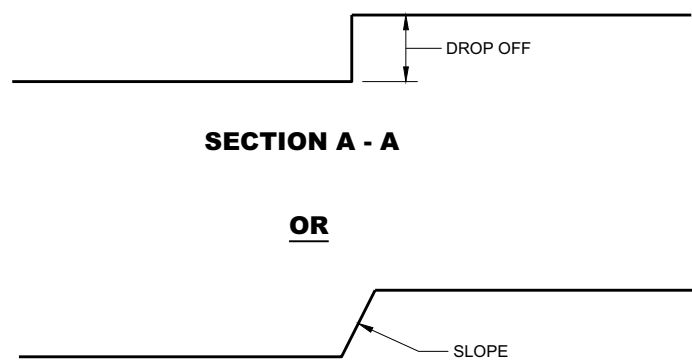
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



MULTI-LANE



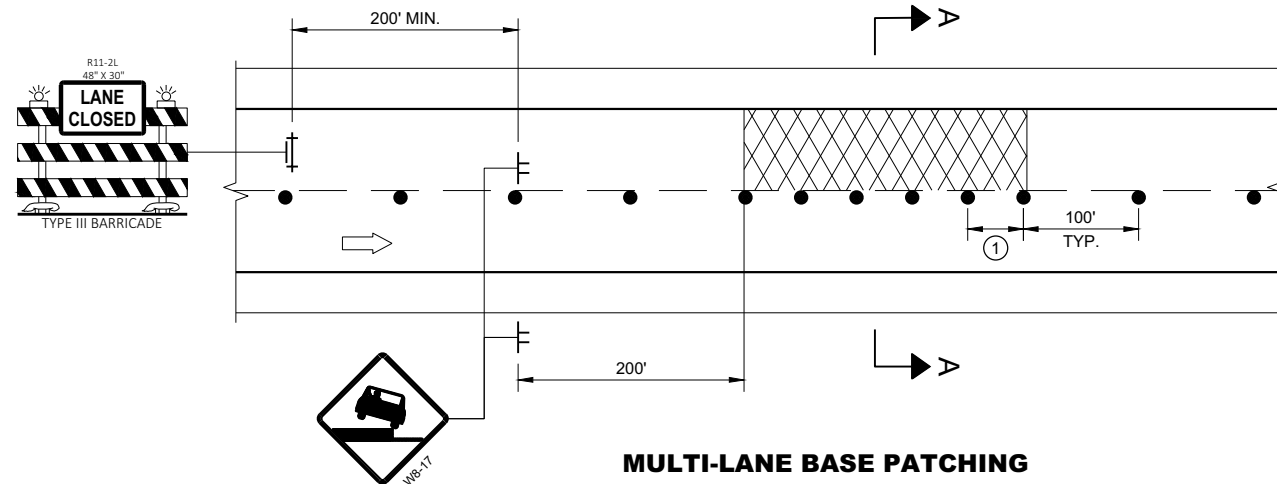
TWO-WAY TWO LANE



SECTION A - A

OR

SECTION A - A



MULTI-LANE BASE PATCHING

ADJACENT LANE DROP-OFFS

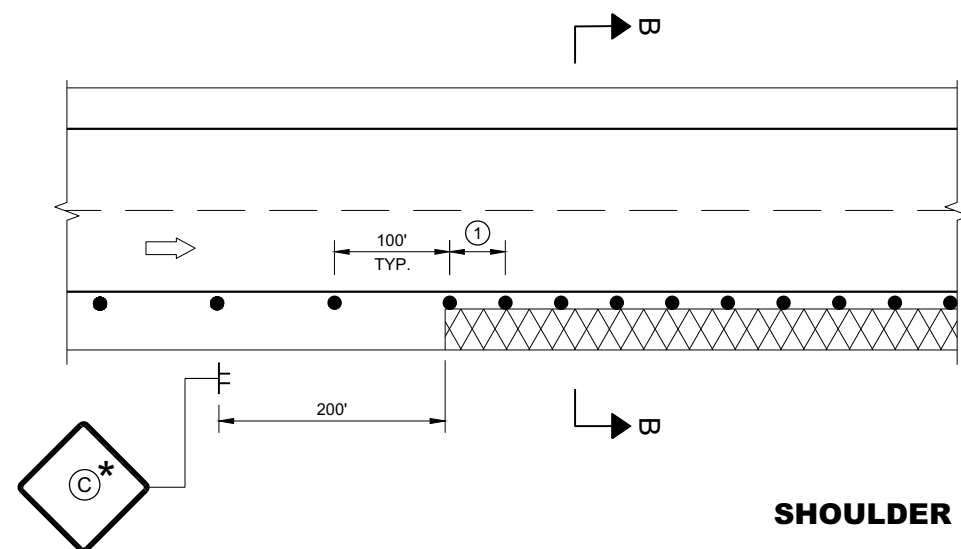
GENERAL NOTES

- FOR SPOT LOCATIONS USE ENGINEERING JUDGEMENT WHEN PLACING ADDITIONAL SIGNS.
- ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.
- "WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.
- WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.
- * IF THE DROP-OFF IS CONTINUOUS ALONG THE PROJECT, PLACE ADDITIONAL SIGNS EVERY 1 MILE AND AFTER EVERY ENTRANCE RAMP.
- ① USE CLOSER SPACING WHEN DELINEATING DROP-OFF.

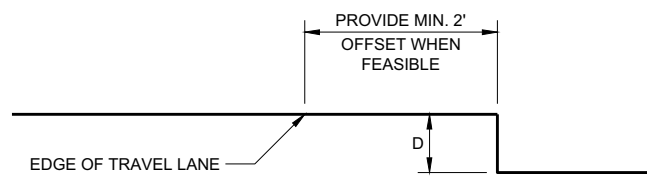
LEGEND

- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL DRUM
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- DIRECTION OF TRAFFIC
- WORK AREA WITH DROP-OFF
- MILLED SURFACE

D	SIGN (C)
< 2" WITH A SLOPE STEEPER THAN 3:1	 LOW SHOULDER WO8-9
2" < 6" WITH A SLOPE STEEPER THAN 3:1	 SHOULDER DROP-OFF W8-9A PROVIDE A 3:1 OR FLATTER SLOPE OF MATERIAL ADJACENT TO THE PAVEMENT



SHOULDER DROP-OFFS



SECTION B - B

**TRAFFIC CONTROL,
DROP-OFF SIGNING**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
March 2018 /S/ Andrew Heidtke
DATE DATE WORK ZONE ENGINEER

FHWA

GENERAL NOTES

DRAWING NOT TO SCALE. ALL SIGNS AND POSTS ON THIS SHEET SHALL BE PAID FOR WITH 'TRAFFIC CONTROL SIGNS' BID ITEM. ALL SIDE ROADS WHICH ARE UNDER CONSTRUCTION OF CURB AND GUTTER AND/OR GRADING SHALL BE ADEQUATELY SIGNED.

ALL SIGNS AND DEVICES SHALL BE IN CONFORMANCE WITH THE WISCONSIN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD). SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISDOT STANDARD SIGN PLATES.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THAT THE BACKGROUND IS ORANGE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.



THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

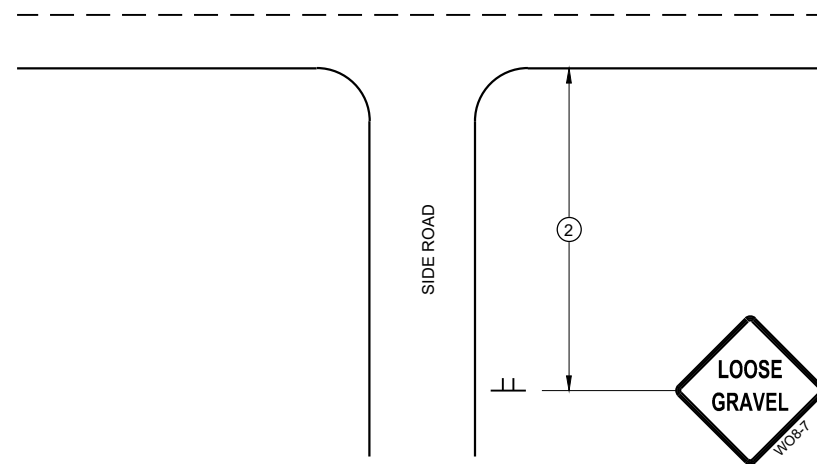
ALL SIGNS INAPPROPRIATE TO THE STATUS OF THE CONTROL ZONE, INCLUDING PRE-EXISTING SIGNS IN THE VICINITY, SHALL BE COVERED OR REMOVED AS DIRECTED BY THE ENGINEER.

SEE 15C34 FOR ADDITIONAL TRAFFIC CONTROL SIGNING WHEN CENTERLINE PAVEMENT MAKINGS ARE MISSING. 'DO NOT PASS' SIGNS MUST BE INSTALLED ON THE SAME DAY AS MILLING OPERATIONS.

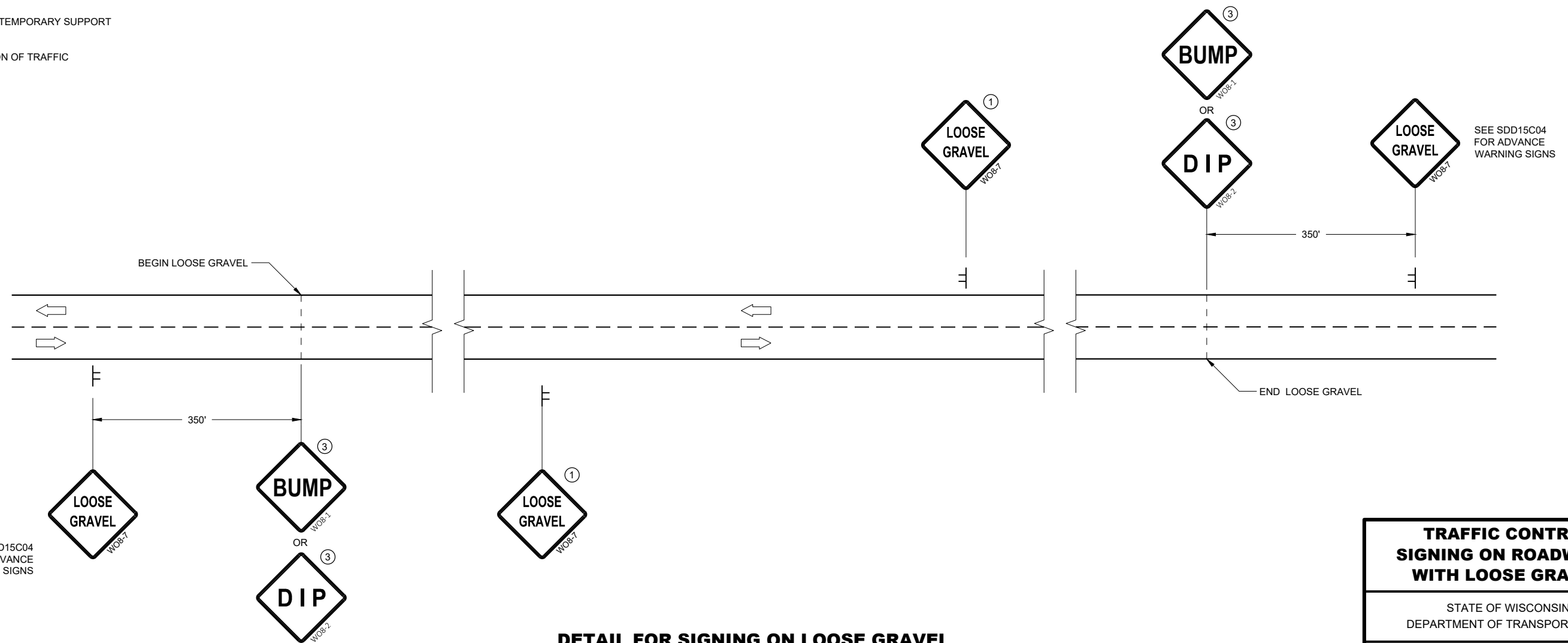
- ① PLACE SIGNS 350' IN ADVANCE OF CHIP SEALED OR LOOSE GRAVEL SURFACES AND AT 1 MILE INTERVALS, OR AS DIRECTED BY THE ENGINEER.
- ② PLACE SIGN 200' MIN. FROM INTERSECTION AND 200' MIN. AFTER ADVANCE WARNING SIGN SHOWN IN SDD 15C04.
- ③ ADD WO8-1 OR WO8-2 SIGN WHEN THE CONDITION IS PRESENT.

LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  DIRECTION OF TRAFFIC



TYPICAL SIDE ROAD APPROACH SIGN DETAIL



DETAIL FOR SIGNING ON LOOSE GRAVEL OR CHIP SEALED SURFACES

SEE SDD15C04 FOR ADVANCE WARNING SIGNS

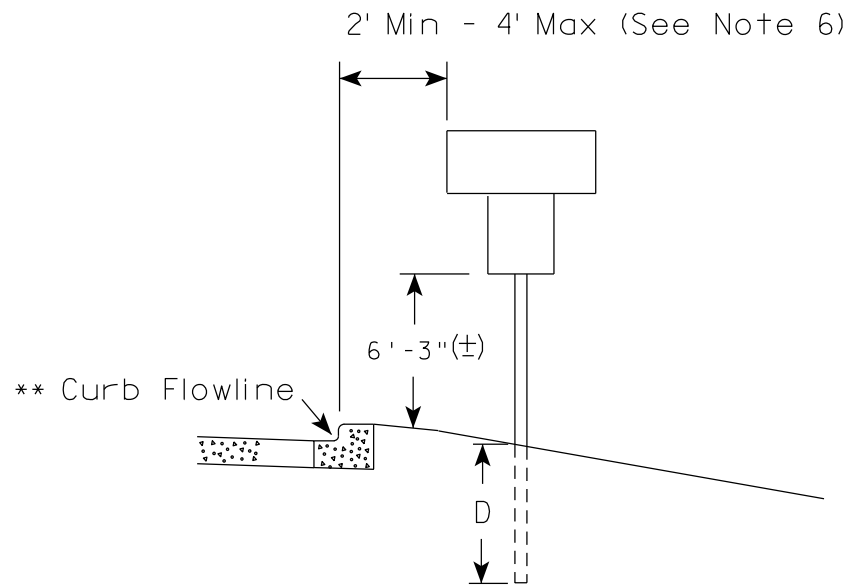
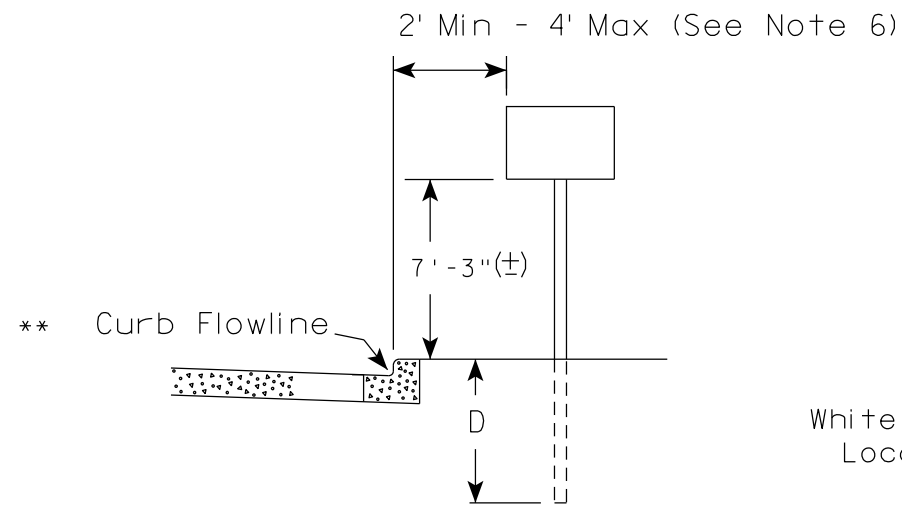
TRAFFIC CONTROL SIGNING ON ROADWAYS WITH LOOSE GRAVEL

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

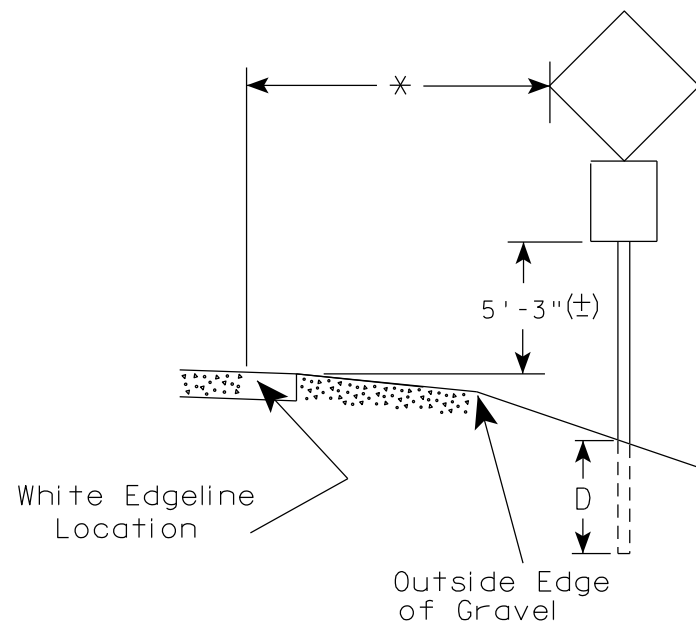
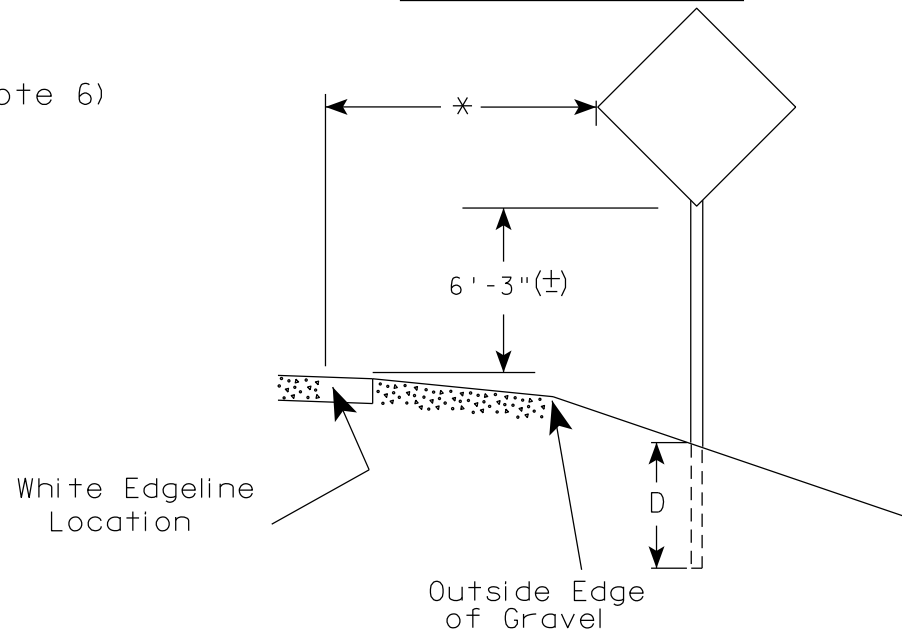
APPROVED
February 2021 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA

URBAN AREA



RURAL AREA (See Note 2)



GENERAL NOTES

1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on or behind barrier wall, see A4-10 sign plate.
The Double Arrow sign (W12-1D) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).
3. For expressways and freeways, mounting height is 7'- 3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
4. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±).
5. Offset distance shall be consistent with existing signs or consistent throughout length of project.
6. The (±) tolerance for mounting height is 3 inches.
7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the Engineer.

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

* * The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 5/13/2020 PLATE NO. A4-3.22



ELEVATION VIEW

DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

- NOTES:**
1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
 2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
 3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



PLAN VIEW

FOR NEW CONCRETE/ ASPHALT INSTALLATIONS

**SIGN POST
BOX-OUTS
A4-3B**

WISCONSIN DEPT OF TRANSPORTATION

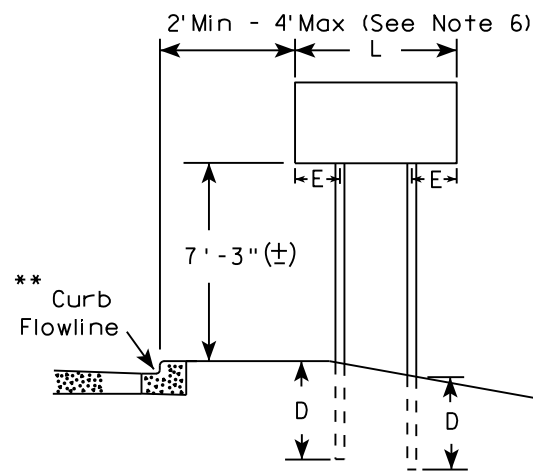
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 1/27/14 PLATE NO. A4-3B.1

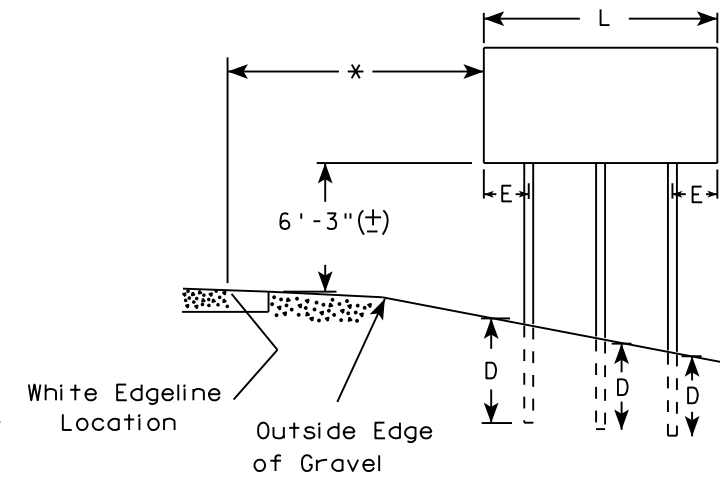
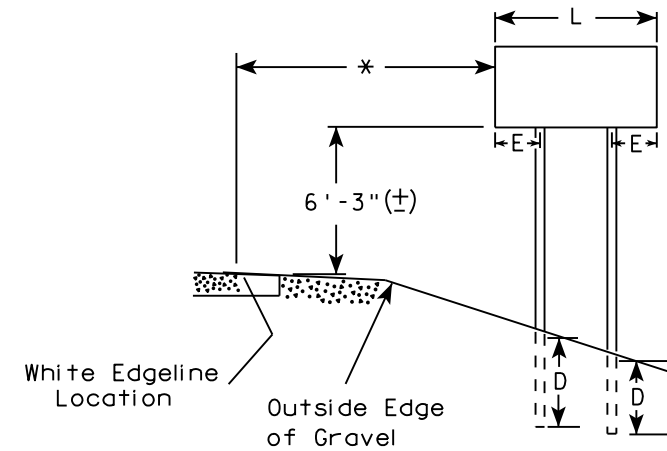
GENERAL NOTES

1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
2. See tables below for required number of posts.
3. For expressways and freeways, mounting height is 7'-3" (±) or 6'-3" (±) depending upon existence of sub-sign.
4. The (±) tolerance for mounting height is 3 inches.
5. J-Assemblies are considered to be one sign for mounting height.
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the engineer.
8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).

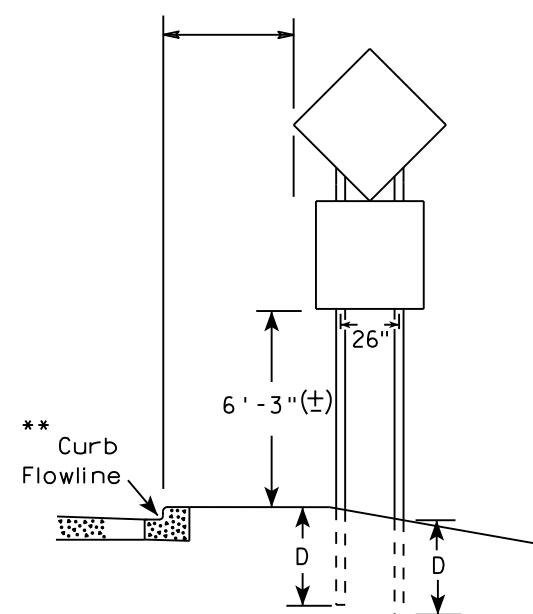
URBAN AREA



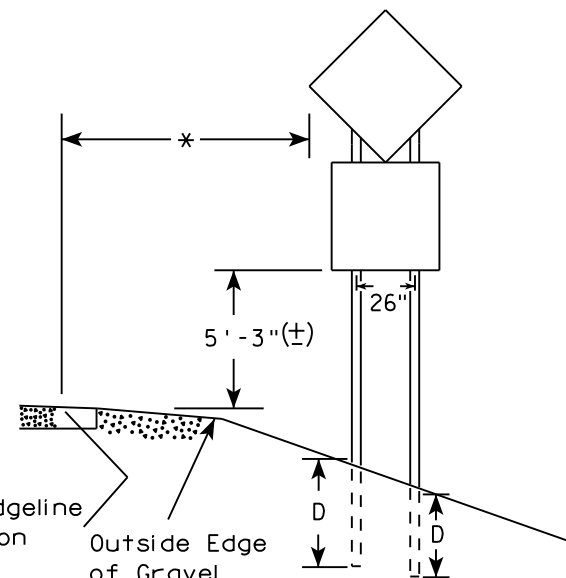
RURAL AREA (See Note 3)



2' Min - 4' Max (See Note 6)



48" DIAMOND WARNING SIGN



48" DIAMOND WARNING SIGN

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

*** See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)	
L	E
Greater than 48" Less than 60"	12"
60" to 108"	L/5

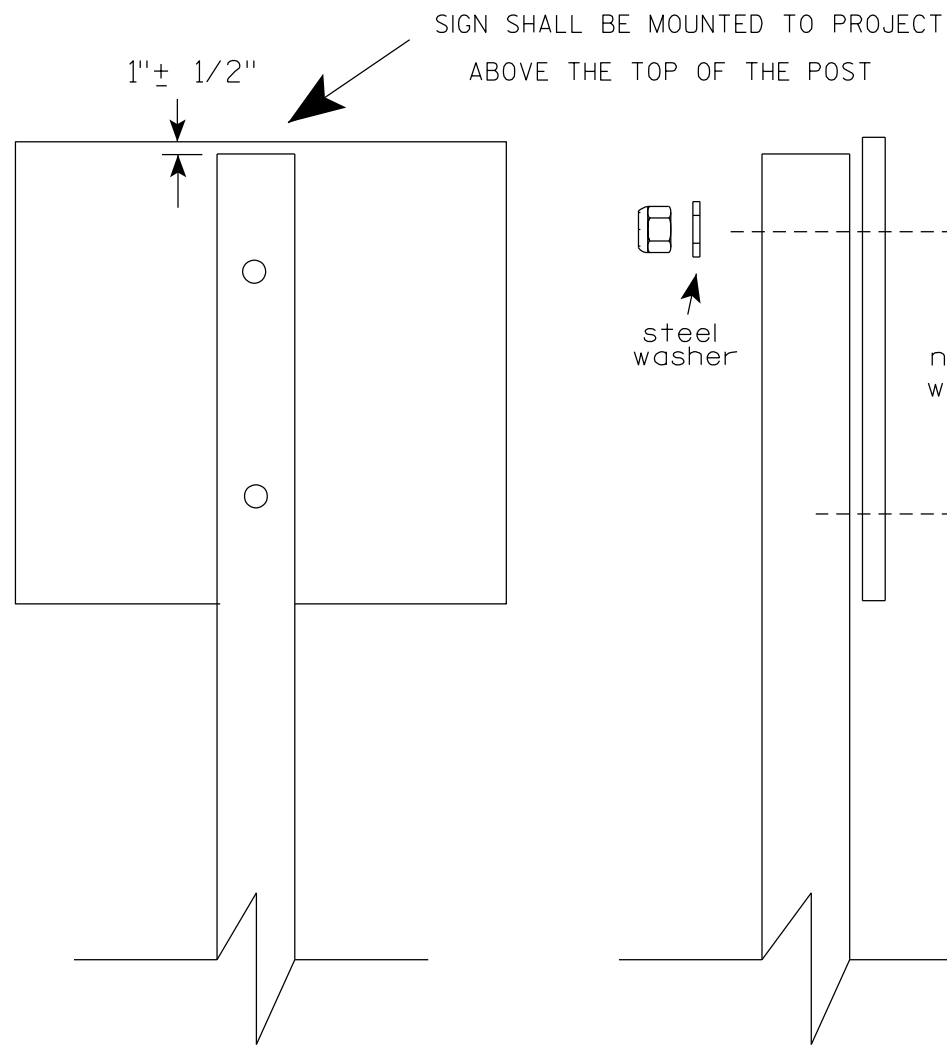
SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)	
L	E
Greater than 108" to 144"	12"

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS

WISCONSIN DEPT OF TRANSPORTATION
 APPROVED *Matthew R. Rauch*
 For State Traffic Engineer
 DATE 8/21/17 PLATE NO. A4-4.15



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

- Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
- Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

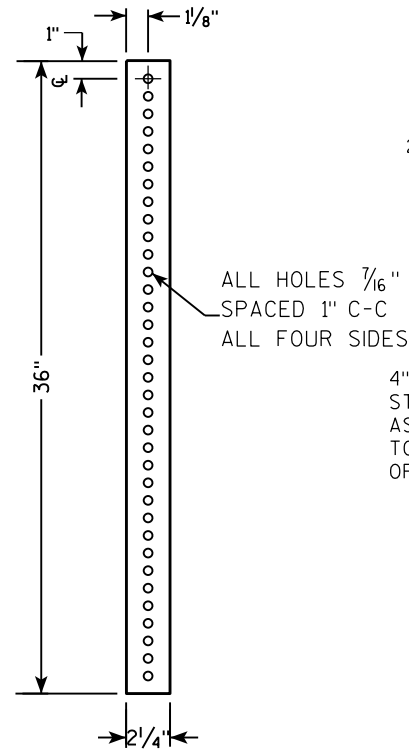
- STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)
- MACHINE BOLTS - $\frac{5}{16}$ " X 1-3/4" Length w/ lock nuts
- WOOD POSTS (4" x 6")
- LAG SCREWS - $\frac{3}{8}$ " X 3" (NO STRINGERS ON BACK OF SIGN)
 $\frac{3}{8}$ " X 4" (STRINGERS ON BACK OF SIGN)
- SQUARE STEEL POSTS (2" x 2")
- MACHINE BOLTS - $\frac{3}{8}$ " X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)
 $\frac{3}{8}$ " X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)
- RIVETS - $\frac{9}{32}$ " (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
 O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH
- WASHERS (ALL POSTS) -
- 1-1/4" O.D. X $\frac{3}{8}$ " I.D. X $\frac{1}{16}$ " STEEL
- 1-1/4" O.D. X $\frac{3}{8}$ " I.D. X .080 NYLON

* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS TO POSTS	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R Rauch</i> For State Traffic Engineer
DATE 4/1/2020	PLATE NO. A4-8.9

**TELESCOPIC TUBING ANCHORS
TWO PIECE SYSTEM**

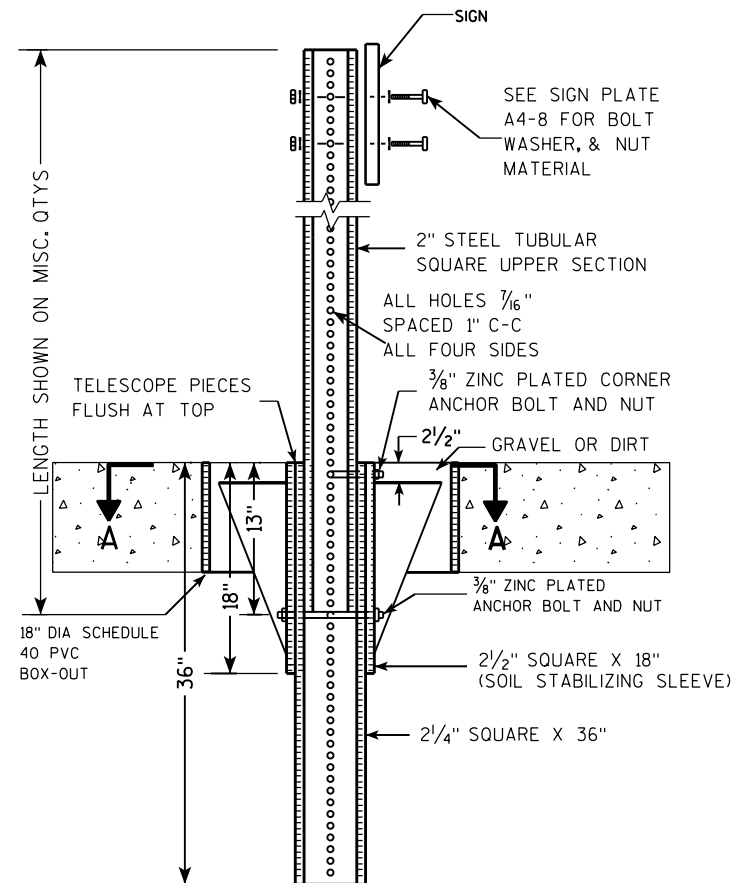
**2 1/4" SQUARE
12 GAUGE
PERFORATED
GALVANIZED FINISH**



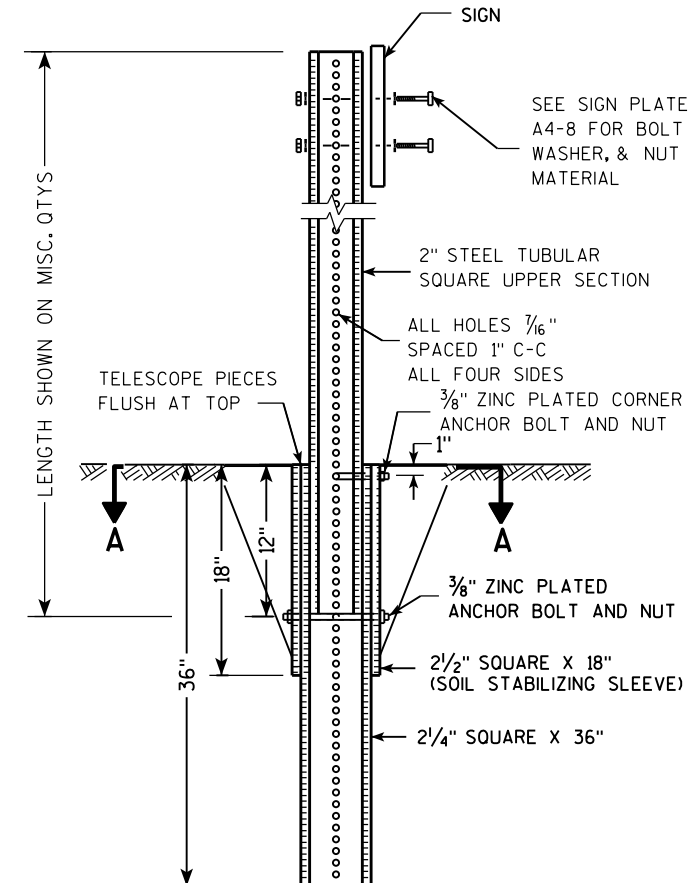
**2 1/2" SQUARE
12 GAUGE
OMNI-DIRECTIONAL
PERFORATED
SOIL STABILIZING SLEEVE
GALVANIZED FINISH**



**DETAIL OF TUBULAR STEEL SIGN POST
(IN POURED CONCRETE OR ASPHALT)**



**DETAIL OF TUBULAR STEEL SIGN POST
(IN LOCATIONS OTHER THAN POURED CONCRETE OR ASPHALT)**



Area of Sign Installation (Sq. Ft.)	Number of Required Posts
9 or less	1
Greater than 9 less than or equal to 18	2
Greater than 18 less than or equal to 27	3

Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

**TUBULAR STEEL
SIGN POST
A4-9**

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 2/05/15 PLATE NO. A4-9.9

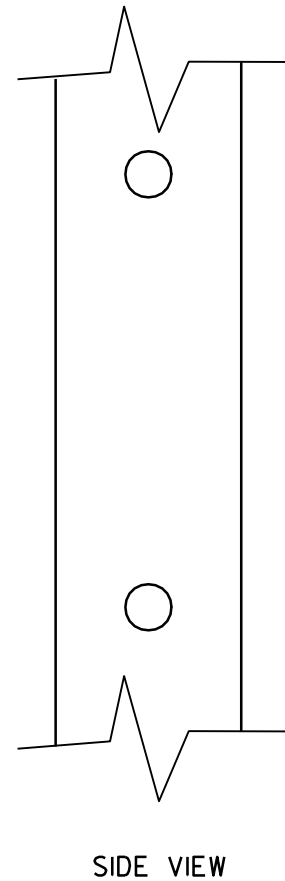
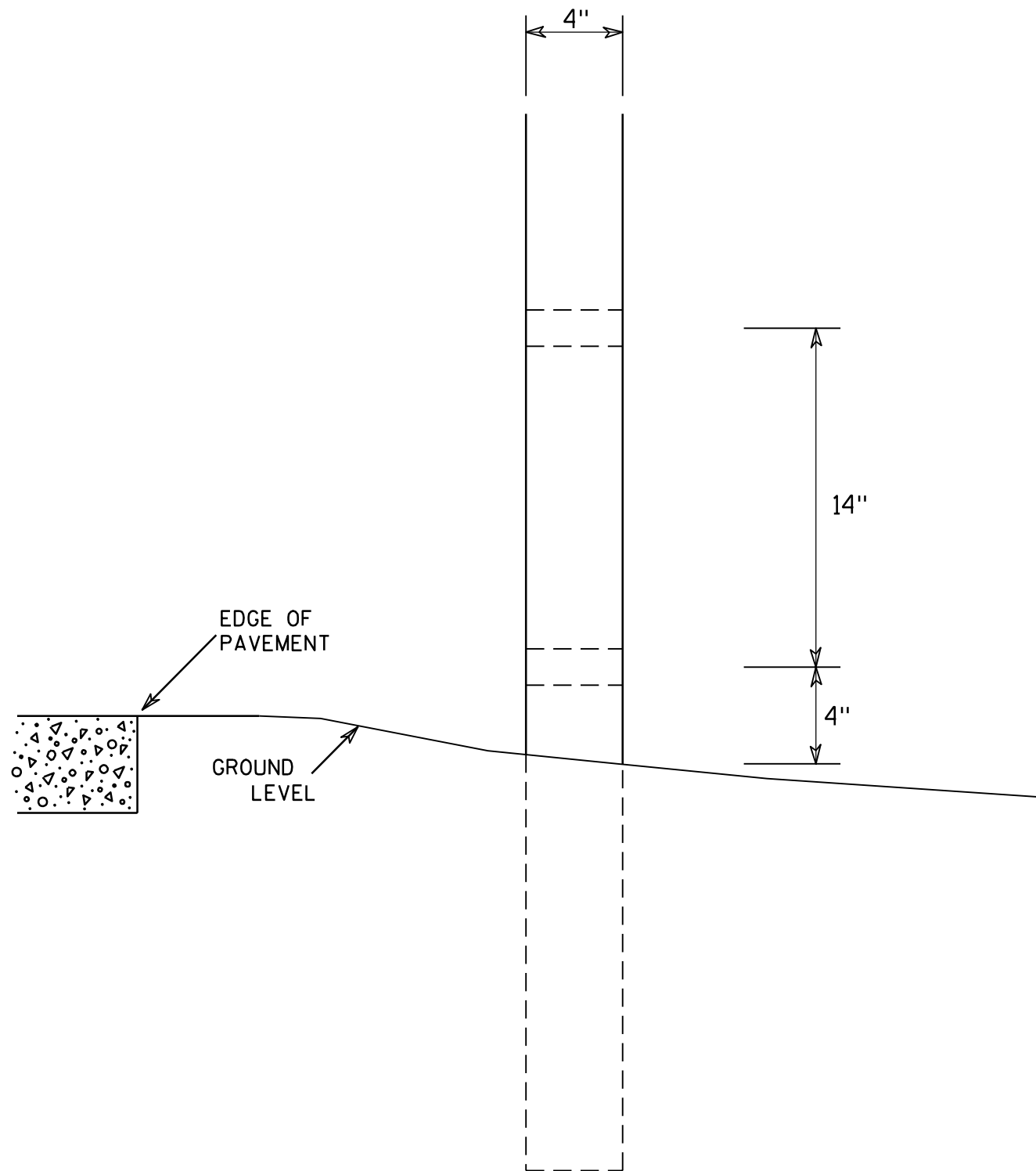
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E



GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

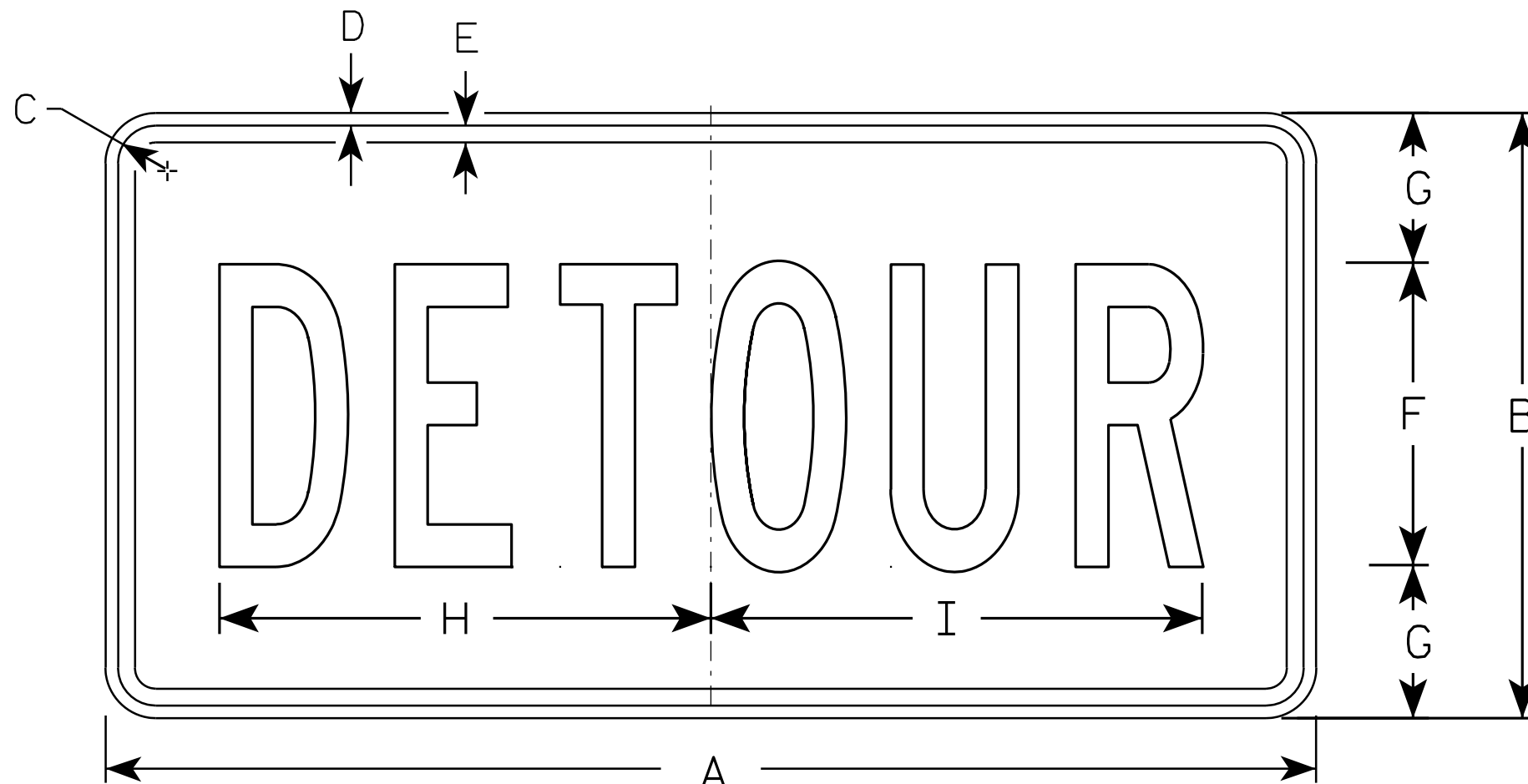
7

7

4 X 6 WOOD POST MODIFICATIONS	
<i>WISCONSIN DEPT OF TRANSPORTATION</i>	
APPROVED	<i>Chester J Spang</i> for State Traffic Engineer
DATE <u>3/27/97</u>	PLATE NO. <u>A4-11.2</u>

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - B
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



M4-8

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	12	1 1/8	3/8	3/8	6	3	10	10 1/4																		2.0
3	36	18	1 1/8	3/8	1/2	9	4 1/2	14 5/8	14 1/2																		4.5
4																											
5																											

STANDARD SIGN
M4-8

WISCONSIN DEPT OF TRANSPORTATION

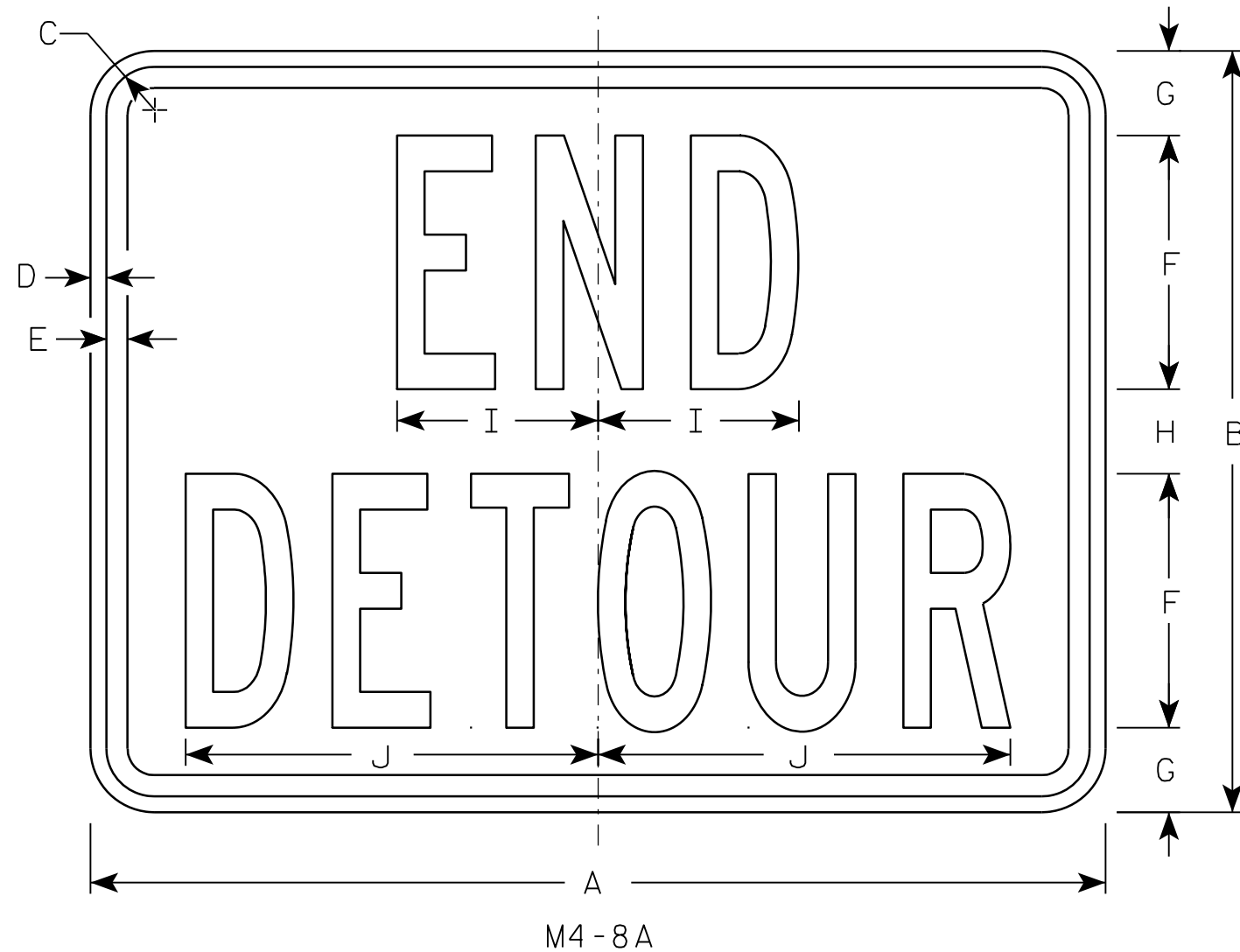
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 11/10/10 PLATE NO. M4-8.2

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - B
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	18	1 1/8	3/8	1/2	6	2	2	4 3/4	9 3/4																	3.0
3	30	24	1 1/8	3/8	1/2	8	2 1/2	3	6 3/4	13																	5.0
4																											
5																											

STANDARD SIGN
M4-8A

WISCONSIN DEPT OF TRANSPORTATION

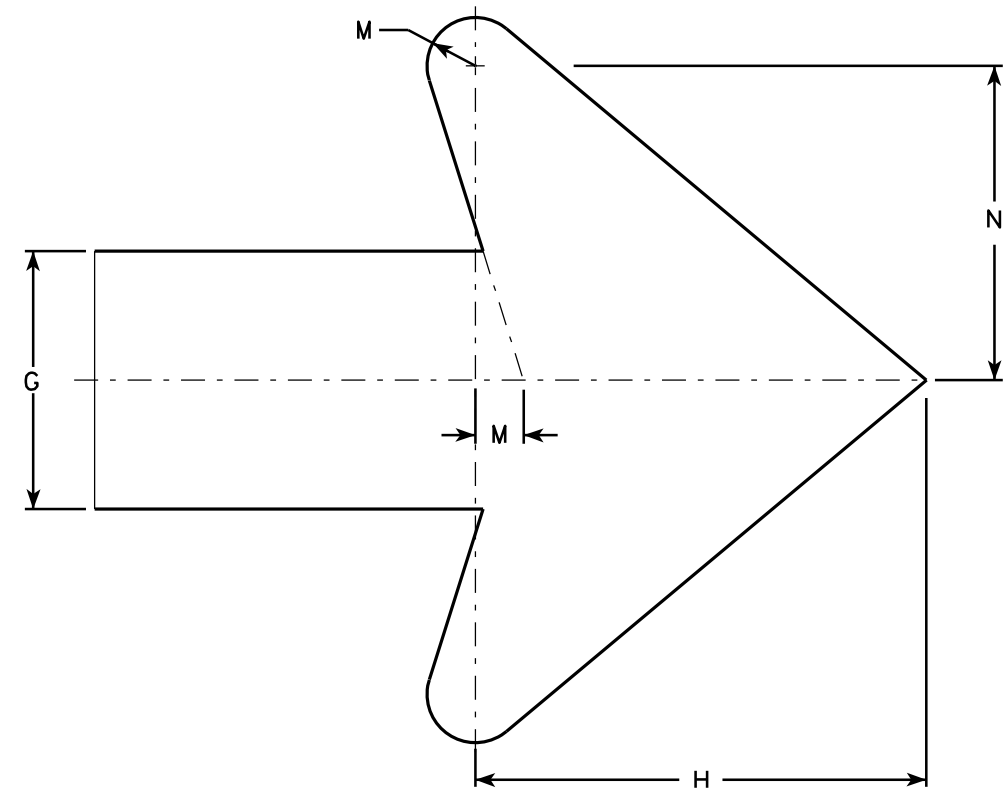
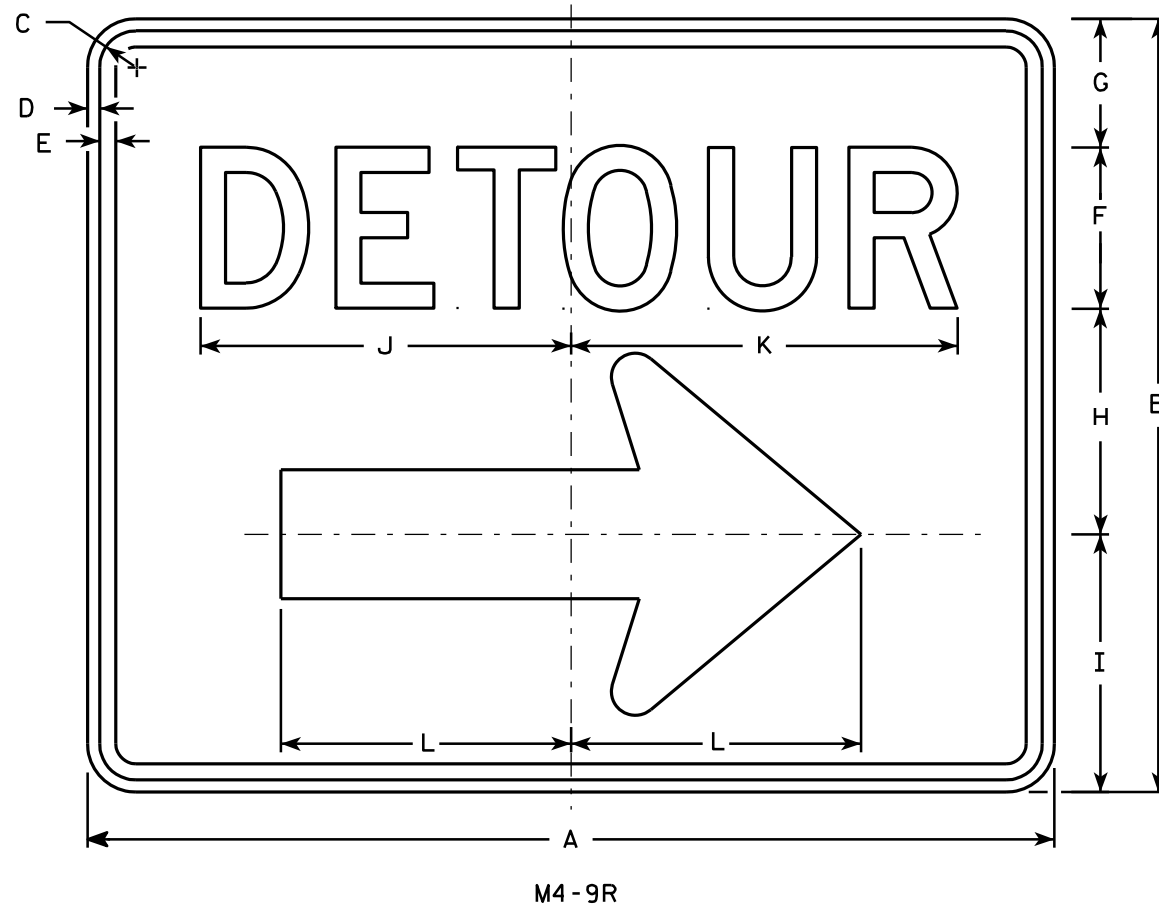
APPROVED *Matthew R Rauch*
for State Traffic Engineer

DATE 3/9/11 PLATE NO. M4-8A.2

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. M4-9L is the same as M4-9R except the arrow is reversed.



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	30	24	1 1/8	3/8	1/2	5	4	7	8	11 1/2	12	9	3/4	4 7/8													5.00
3	30	24	1 1/8	3/8	1/2	5	4	7	8	11 1/2	12	9	3/4	4 7/8													5.00
4	48	36	1 3/8	1/2	5/8	8	6	10 1/2	11 5/8	20 5/8	20 1/2	13 1/4	1 1/8	6 7/8													12.0
5	48	36	1 3/8	1/2	5/8	8	6	10 1/2	11 5/8	20 5/8	20 1/2	13 1/4	1 1/8	6 7/8													12.0

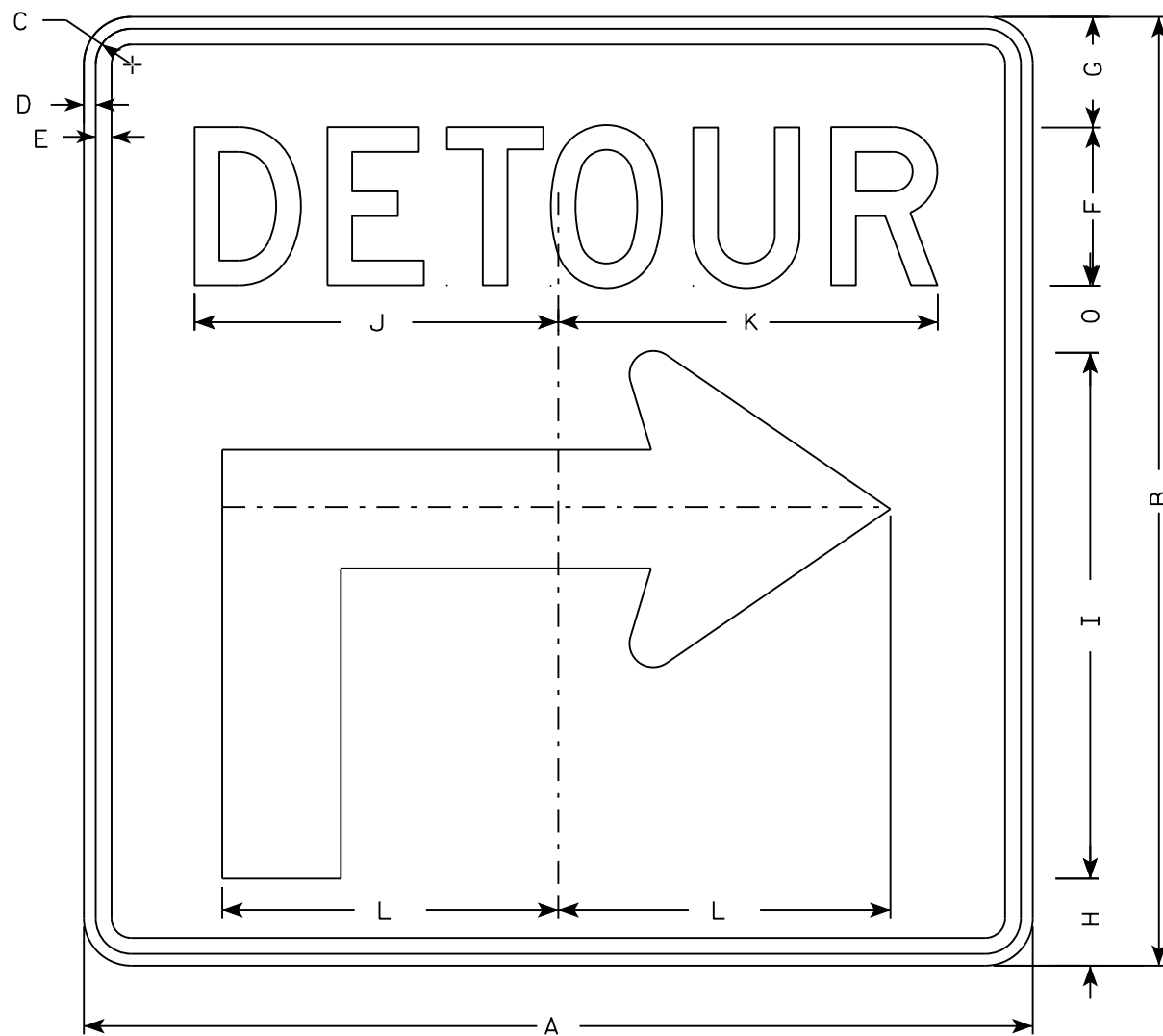
STANDARD SIGN
M4-9 R & L

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 3/9/11 PLATE NO. M4-9R.4

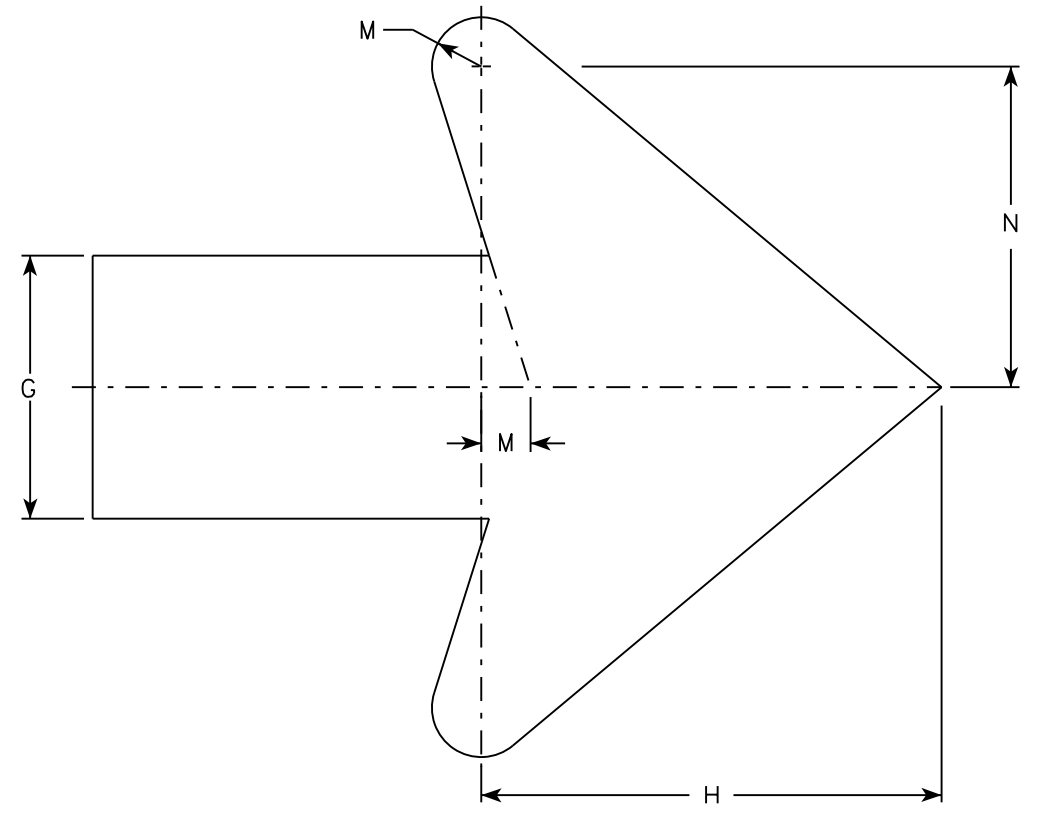
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**



M4-59R

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown when base material is metal.
5. M4-59L is the same as M4-59R except the arrow is reversed.



Arrow Detail

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	30	30	1 1/8	3/8	1/2	5	3 1/2	2 3/4	16 5/8	11 1/2	12	10 1/2	3/4	4 7/8	2 1/8												6.25
3	30	30	1 1/8	3/8	1/2	5	3 1/2	2 3/4	16 5/8	11 1/2	12	10 1/2	3/4	4 7/8	2 1/8												6.25
4	48	48	1 3/8	1/2	5/8	8	5 5/8	4 3/8	26 5/8	20 5/8	20 1/2	17	1 1/8	6 7/8	3 3/8												16.0
5	48	48	1 3/8	1/2	5/8	8	5 5/8	4 3/8	26 5/8	20 5/8	20 1/2	17	1 1/8	6 7/8	3 3/8												16.0

STANDARD SIGN
M4-59 L&R

WISCONSIN DEPT OF TRANSPORTATION

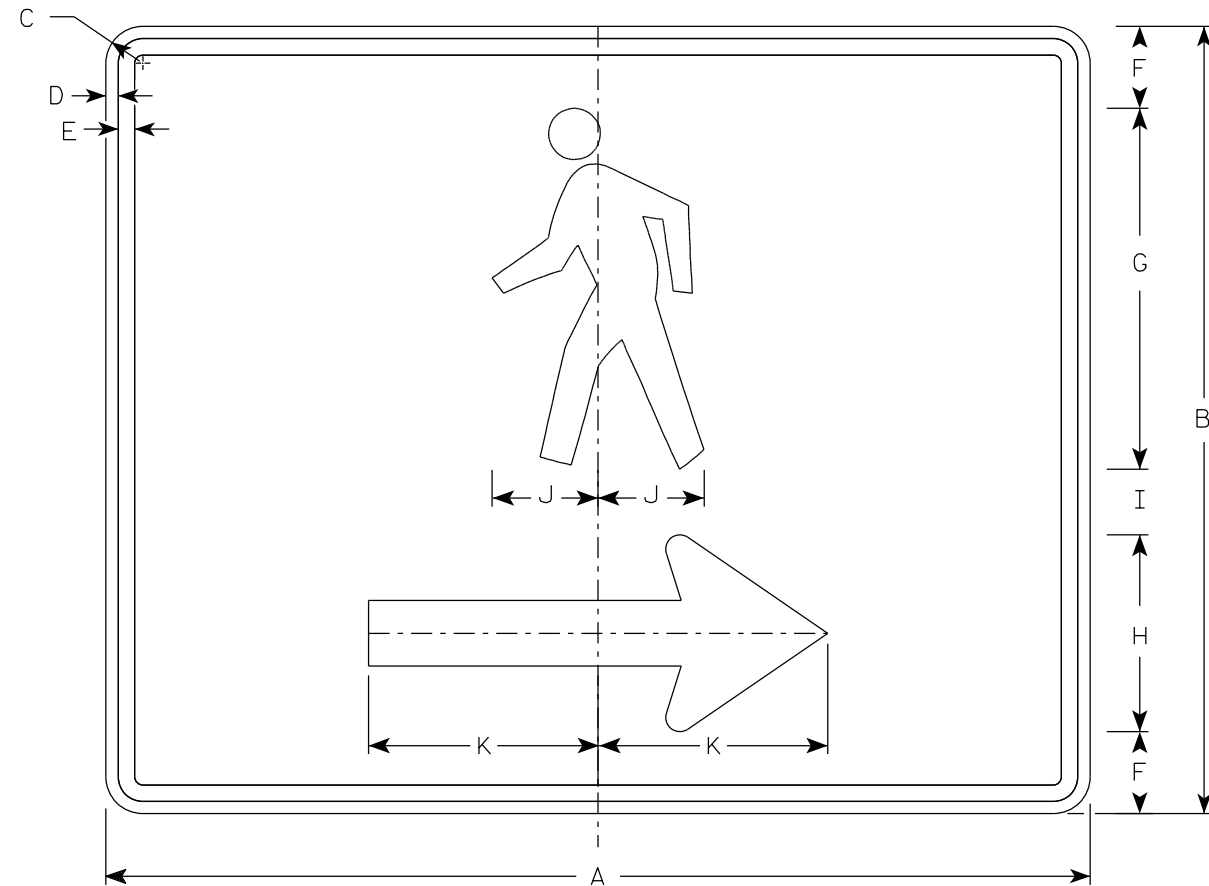
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 11/10/15 PLATE NO. M4-59.1

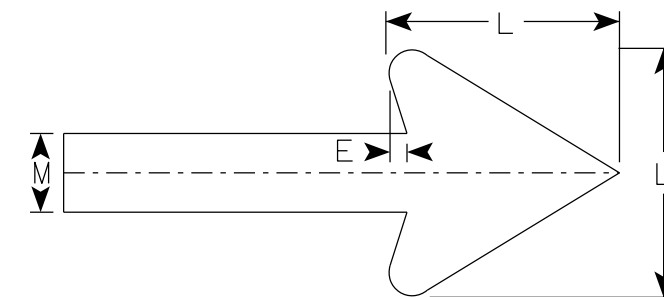
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II- Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. M4-60L is the same as M4-60R except the arrow is reversed.



M4-60R



Arrow Detail

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	30	24	1 1/8	3/8	1/2	2 1/2	11	6	2	3 1/4	7	6	2														5.00
3																											
4																											
5																											

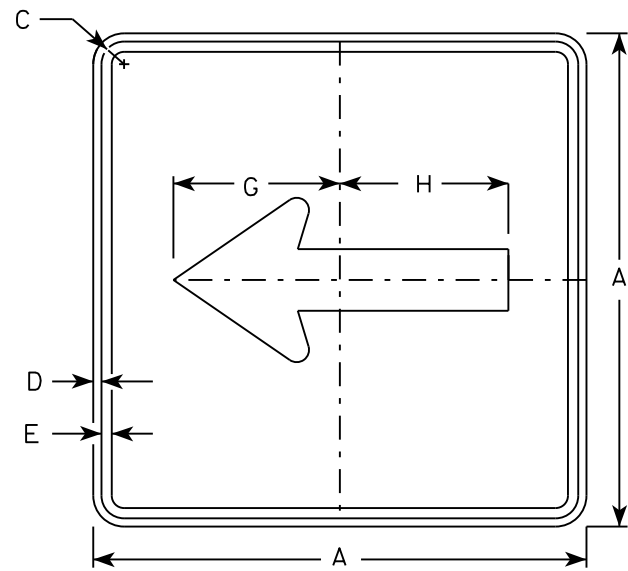
STANDARD SIGN
M4-60 L&R

WISCONSIN DEPT OF TRANSPORTATION

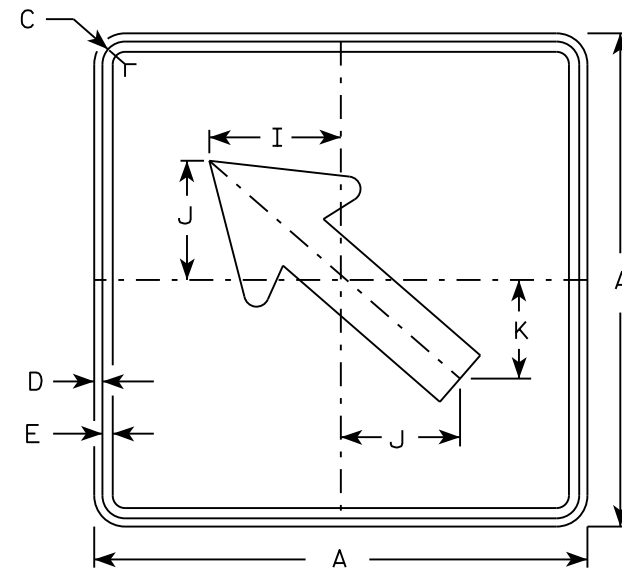
APPROVED *Matthew R Rauch*
For State Traffic Engineer

DATE 9/16/2021 PLATE NO. M4-60.1

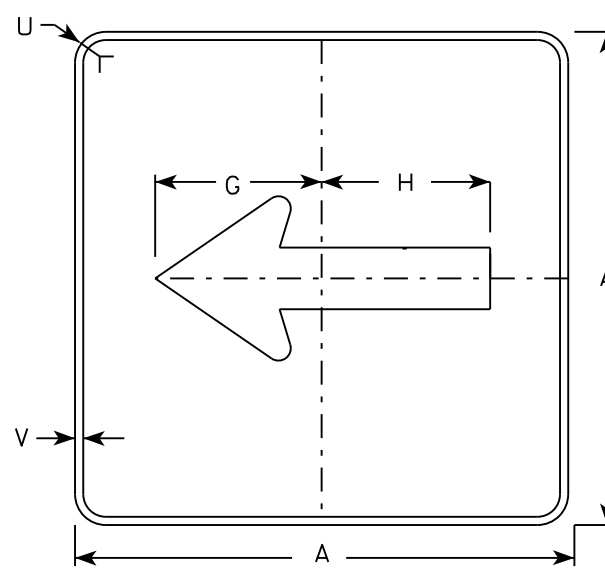
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**



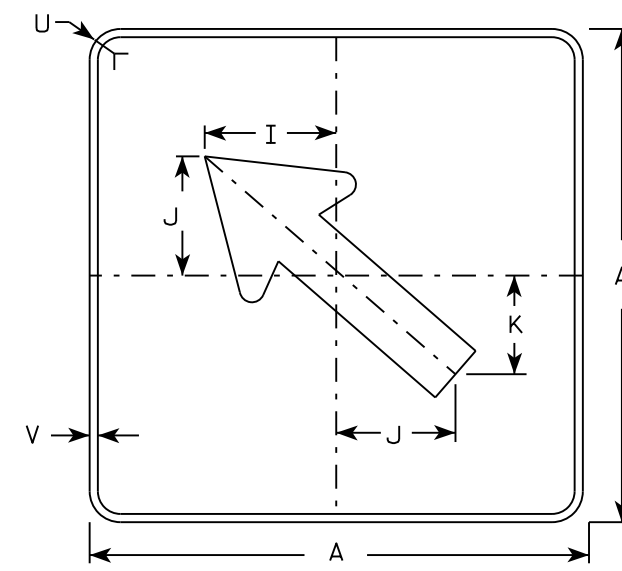
M6-1
MM6-1
M06-1
MP6-1



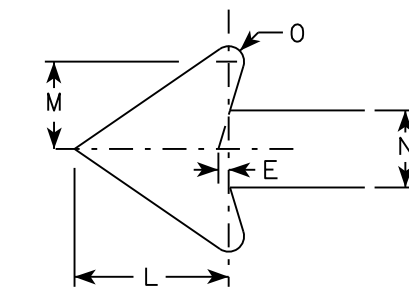
M6-2
MM6-2
M06-2
MP6-2



MB6-1
MK6-1
MN6-1
MR6-1



MB6-2
MK6-2
MN6-2
MR6-2



NOTES

- Signs are Type II - Type H except as Shown
- Color:
Background - See note 4
Message - See note 4
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- M6-1 and M6-2 Background - White
Message - Black
MB6-1 and MB6-2 Background - Blue
Message - White
MK6-1 and MK6-2 Background - Green
Message - White
MM6-1 and MM6-2 Background - White
Message - Green
MN6-1 and MN6-2 Background - Brown
Message - White
M06-1 and M06-2 Background - Orange - Type F Reflective
Message - Black
MP6-1 and MP6-2 Background - White
Message - Blue
MR6-1 and MR6-2 Background - Brown
Message - Yellow

7

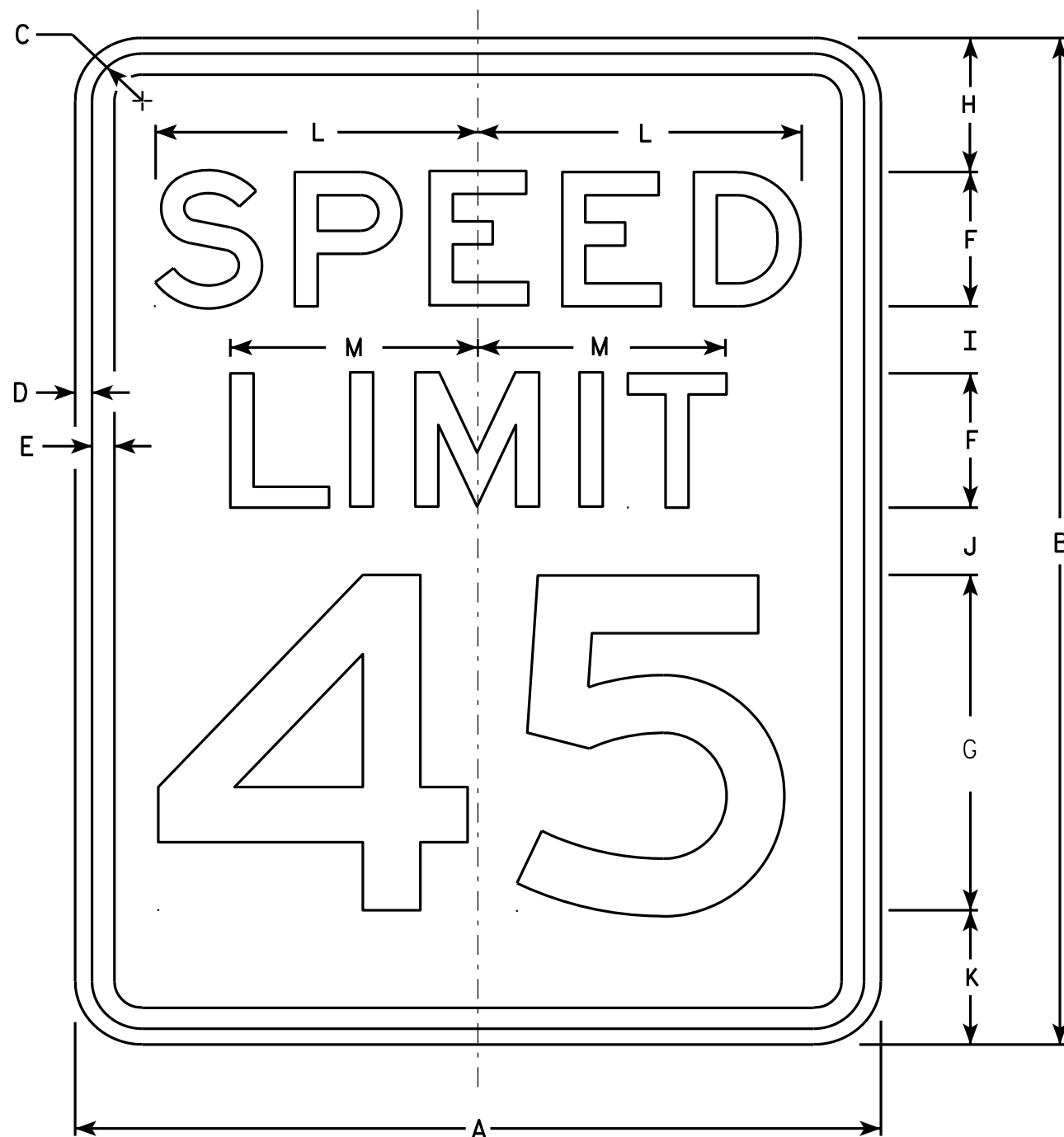
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7 1/2	7 1/8	5 5/8	5	4 1/4	5 1/4	3	2 5/8	1/2							1 1/2	1/2				3.06
3	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4							1 7/8	1/2				6.25
4	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4							1 7/8	1/2				6.25
5	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4							1 7/8	1/2				6.25

STANDARD SIGN
M6-1 & M6-2
SERIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 10/15/15 PLATE NO. M6-1.15



R2-1

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - Black
3. Message Series - E
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals and optically adjust spacing to achieve proper balance.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18	24	1 1/8	3/8	1/2	3	8	3	2	2	3	7 1/4	5 1/2														3.0
2S	24	30	1 1/8	3/8	1/2	4	10	3	2 1/4	3 3/8	3 3/8	9 5/8	7 3/8														5.0
2M	30	36	1 3/8	1/2	5/8	5	12	5	2 1/2	2 1/2	4	12	9 1/4														7.5
3	36	48	1 3/8	1/2	5/8	6	14	6	5	5	6	14 3/8	11														12.0
4	36	48	1 3/8	1/2	5/8	6	14	6	5	5	6	14 3/8	11														12.0
5	48	60	2 1/4	3/4	1	8	20	6	4 1/2	6 3/4	6 3/4	19 1/4	14 5/8														20.0

STANDARD SIGN
R2-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 5/26/10 PLATE NO. R2-1.13

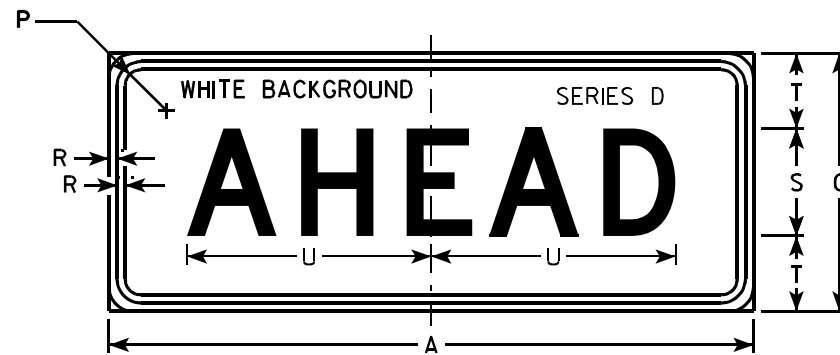
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E



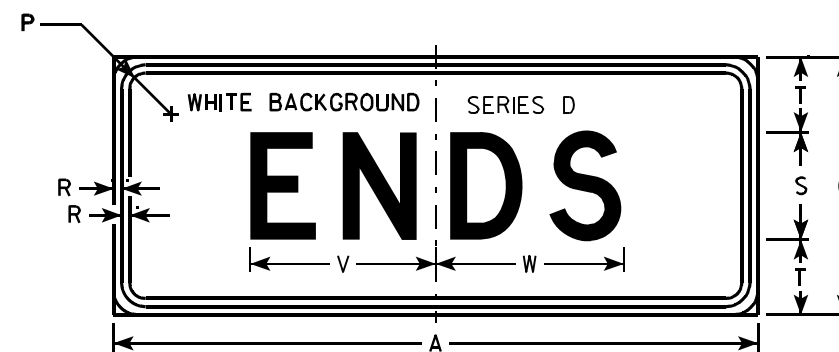
R3-17

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - AS SHOWN
Message - BLACK
3. Message Series - C or as noted on the Signs.
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



R3-17ap



R3-17bp

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	R3-17 Area sq. ft.	R3-17ap Area sq. ft.	R3-17bp Area sq. ft.
1																													
2S	30	24	1 1/8	3/8	1/2	2	4	4 1/8	7 7/8	6 3/8	9 1/2	2 5/8	7/8	13	12	1 1/8	3 3/8	3/8	5	3 1/2	11 3/8	8 5/8	8 3/4	2 3/8	15 5/8	8	5.0	2.5	2.5
2M	30	24	1 1/8	3/8	1/2	2	4	4 1/8	7 7/8	6 3/8	9 1/2	2 5/8	7/8	13	12	1 1/8	3 3/8	3/8	5	3 1/2	11 3/8	8 5/8	8 3/4	2 3/8	15 5/8	8	5.0	2.5	2.5
3																													
4																													
5																													

STANDARD SIGN
R3-17 & R3-17a&bp

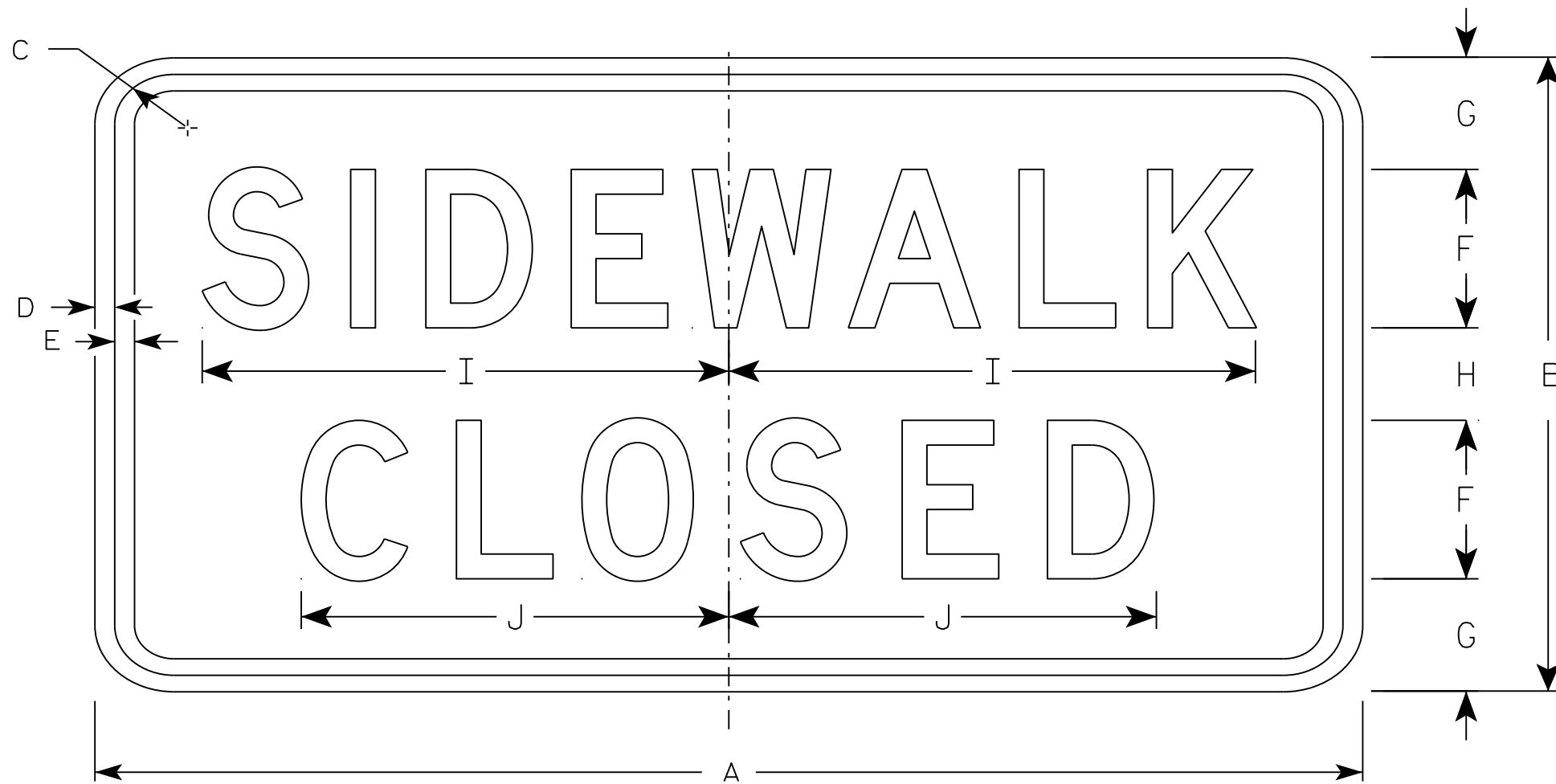
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 4/12/2011 PLATE NO. R3-17.2

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Use Size 2 for Sidewalks. Use Size 3 for Paths and Trails.



R9-9

7

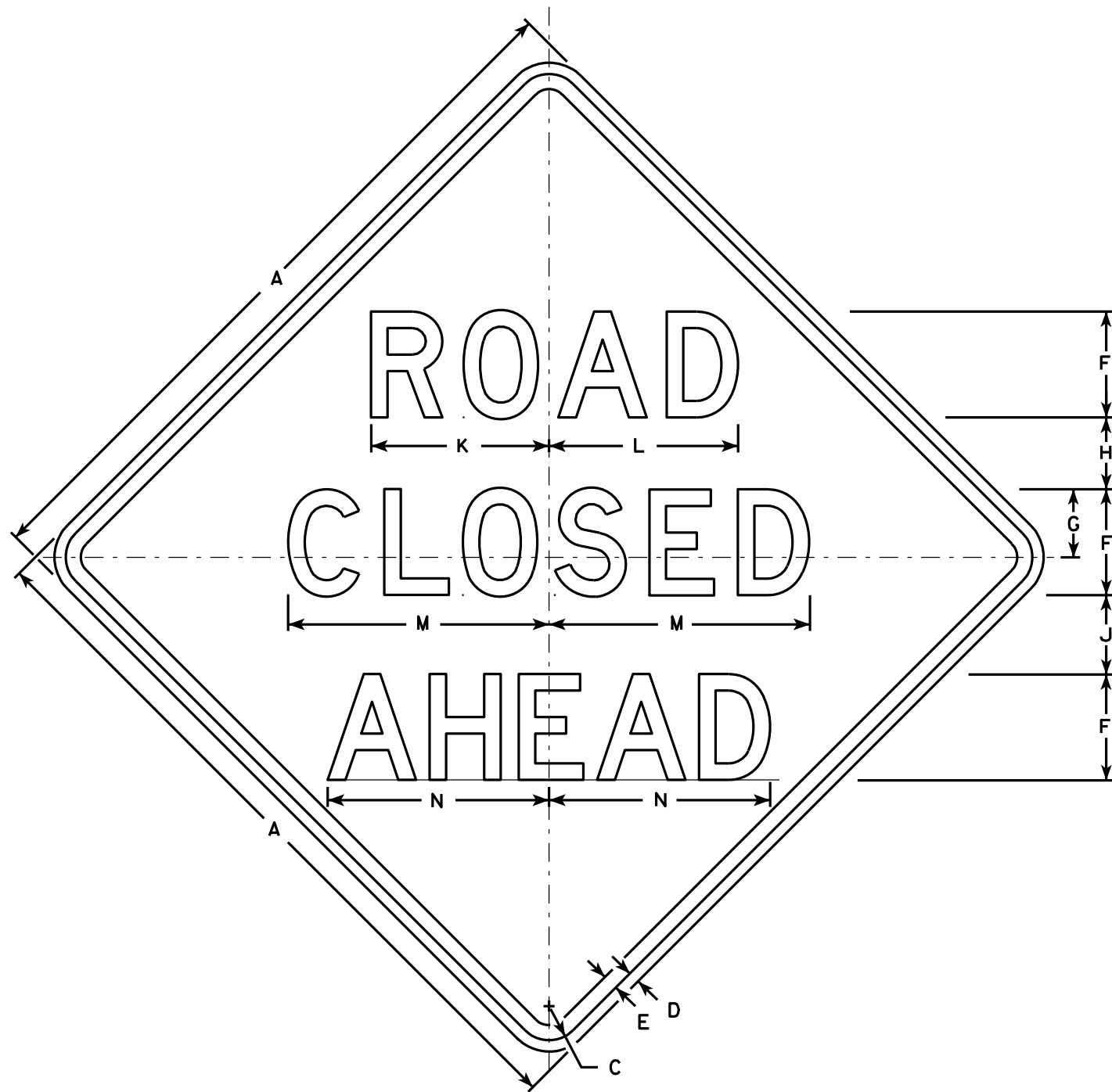
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	12	1 3/4	1/2	1/2	3	2 1/8	1 3/4	10	8 1/8																	2.0
2M	24	12	1 3/4	1/2	1/2	3	2 1/8	1 3/4	10	8 1/8																	2.0
3	30	18	1 3/4	1/2	1/2	4	3 1/2	3	12 1/2	10 1/4																	3.75
4																											
5																											

STANDARD SIGN
R9-9

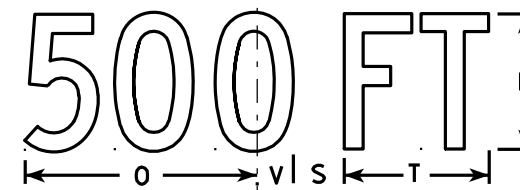
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

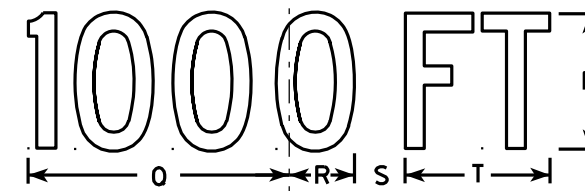
DATE 8/11/16 PLATE NO. R9-9.6



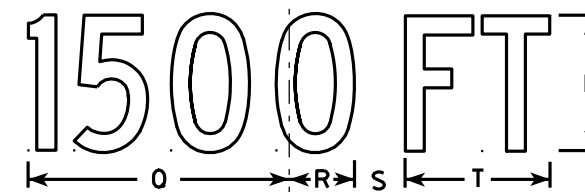
W20-3A



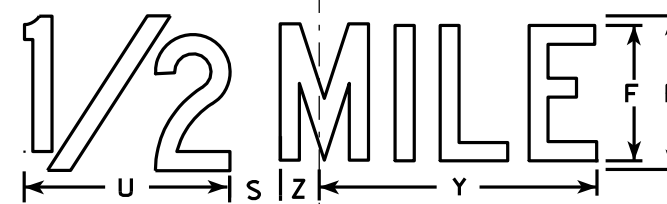
W20-3D



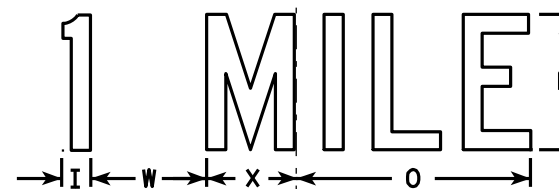
W20-3C



W20-3B



W20-3G



W20-3F

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - see note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Lines 1 and 2 are Series D.
Line 3 is Series D for AHEAD and Series C for all other distances.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	5	3 3/8	3 1/2	1 1/8	4	8 3/8	8 7/8	12 1/2	11	9	6	10 1/8	2 1/2	1 7/8	5 5/8	8	1 3/8	4 1/2	3 1/2	10 3/4	1 3/4	9.0
2S	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
2M	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
3	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
4	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
5	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0

STANDARD SIGN
W20-3A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 3/18/11 PLATE NO. W20-3.7

Notes



Wisconsin Department of Transportation

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