LAX

DECEMBER 2022

PROJECT ID: 1620-02-78

WITH: N/A

COUNTY: JUNEAU

ORDER OF SHEETS

TOTAL SHEETS = 142

01

DESIGN DESIGNATION

A.A.D.T.	(2023)	=	1150
A.A.D.T.	(2043)	=	1250
D.H.V.		=	31.2
D.D.		=	0.5
T.		=	37.1%
DESIGN SPEED		=	60 MPH (RURAL)/30-50 MPH (URBAN)
ESALS		=	1,100,000

CONVENTIONAL SYMBOLS

PLAN

- CORPORATE LIMITS
- PROPERTY LINE
- LOT LINE
- LIMITED HIGHWAY EASEMENT
- EXISTING RIGHT OF WAY
- PROPOSED OR NEW R/W LINE
- SLOPE INTERCEPT
- REFERENCE LINE (300'EB')
- EXISTING CULVERT
- PROPOSED CULVERT (Box or Pipe)
- COMBUSTIBLE FLUIDS (CAUTION)
- MARSH AREA
- WOODED OR SHRUB AREA

PROFILE

- GRADE LINE
- ORIGINAL GROUND
- MARSH OR ROCK PROFILE (To be noted as such) (ROCK)
- SPECIAL DITCH (LABEL)
- GRADE ELEVATION (95.36)
- CULVERT (Profile View)

UTILITIES

- ELECTRIC (E)
- FIBER OPTIC (FO)
- GAS (G)
- SANITARY SEWER (SAN)
- STORM SEWER (SS)
- TELEPHONE (T)
- WATER (W)
- UTILITY PEDESTAL
- POWER POLE
- TELEPHONE POLE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

NECEDAH - BABCOCK

STH 21 TO COUNTY LINE RD

STH 80

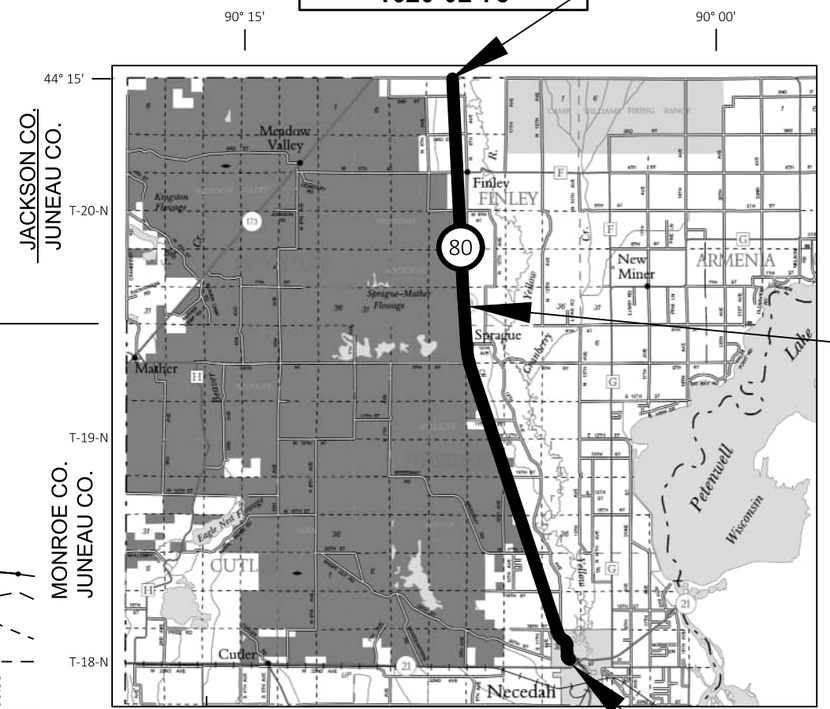
JUNEAU COUNTY

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
1620-02-78	WISC 2023084	1

STATE PROJECT NUMBER

1620-02-78

END PROJECT
STA 1073+28
Y = 321720.766
X = 446343.658



STRUCTURE
B-29-159

BEGIN PROJECT
STA 230+32
Y = 240531.913
X = 463308.100

LAYOUT

SCALE 0 — 4 MI

TOTAL NET LENGTH OF CENTERLINE = 15.965 MI

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COUNTY COORDINATES, JUNEAU COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.
ELEVATIONS SHOWN IN THIS PLAN ARE REFERENCED TO THE NORTH AMERICAN VERTICAL DATUM OF 1988, NAVD88 (2012).

ORIGINAL PLANS PREPARED BY

wsp

831 Critter Court
Suite 400
Onalaska, WI 54650
Phone: (608) 519-1455

WISCONSIN
RYAN B. McKANE
E-37669
HOLMEN WI.
PROFESSIONAL ENGINEER

Ryan B McKane

7/13/2022 (Date) (Signature)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY

Surveyor	WISDOT
Designer	KNIGHT E/A
Project Manager	JERED LEX
Regional Examiner	SW REGION
Regional Supervisor	DAN KLEINERTZ

APPROVED FOR THE DEPARTMENT

DATE: 7/14/2022

Jered Lex — Digitally signed by Jered Lex, DN: CN=JE, E=Jered.lex@dot.wi.gov, OU=WisDOT, C=US, Date: 2022.07.14 09:57:46 -05'00' (Signature)

E

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\010101_TI.DWG PLOT DATE : 7/13/2022 3:14 PM PLOT BY : KAKACEK, JOHN PLOT NAME :

GENERAL NOTES

- ALL RADII ARE MEASURED TO EDGE OF PAVEMENT UNLESS OTHERWISE SHOWN OR NOTED ON THE PLAN.
- ALL SIGN LOCATIONS SHALL BE REVIEWED BY THE ENGINEER PRIOR TO INSTALLATION. ANY CONFLICTING SIGNS SHALL BE COVERED OR REMOVED.
- TYPICAL SECTIONS SHOW THE GENERAL FEATURES THROUGHOUT THE PROJECT. PAVEMENT SLOPES, TERRACE SLOPES, ETC., MAY VARY WITHIN THE LIMITS OF THE SECTION.
- NO TREES OR SHRUBS SHALL BE REMOVED UNLESS SUCH TREES OR SHRUBS HAVE BEEN DESIGNATED FOR REMOVAL BY THE ENGINEER.
- DISTURBED AREAS WITHIN THE RIGHT OF WAY ARE TO BE TOPSOILED, FERTILIZED, SEEDED, AND MULCHED OR E-MAT AS DIRECTED BY THE ENGINEER.
- CONTRACTOR SHALL MAINTAIN ACCESS TO ALL DRIVEWAYS AT ALL TIMES. IN SITUATIONS WHEN SIGNIFICANT WORK OPERATIONS ARE IMMEDIATELY IN FRONT OF DRIVEWAY, OWNER SHOULD BE CONTACTED PRIOR TO REMOVAL SO ARRANGEMENTS CAN BE MADE.
- WHEN PORTIONS OF EXISTING ASPHALTIC SURFACES ARE TO BE REMOVED TO ACCOMMODATE NEW CONSTRUCTION, THE LINE OF SUCH REMOVAL SHALL BE NEATLY DELINEATED WITH A SAW CUT JOINT THROUGH THE ASPHALTIC SURFACE SO THAT REMOVAL OF THE ASPHALT SHALL BE ACCOMPLISHED WITHOUT DAMAGE TO REMAINING PORTIONS. THE LOCATION OF SAW JOINTS AND THE AMOUNT REMOVED AT SIDE ROADS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- CROSS SLOPES AS SHOWN ON THE TYPICAL SECTION WILL VARY AT THE INTERSECTIONS.
- THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.
- WHEN THE QUANTITY OF BASE AGGREGATE IS MEASURED FOR PAYMENT BY THE TON OR CUBIC YARD, THE DEPTH OR THICKNESS OF THE LAYER SHOWN ON THE PLANS IS APPROXIMATE AND THE ACTUAL THICKNESS WILL DEPEND ON THE DISTRIBUTION OF THE MATERIAL AS DIRECTED BY THE ENGINEER.
- THE CONTRACTOR'S PAVING OPERATIONS SHALL BE CONSISTENT WITH THE PLAN TYPICAL SECTIONS AND CONSTRUCTED TO PREVENT HMA LONGITUDINAL JOINTS FROM BEING LOCATED WITHIN A DRIVING, TURNING, PASSING, OR PARKING LANE.
- APPLY TACK COAT AT A RATE OF 0.07 GAL/SY TO MILLED SURFACES AND 0.05 GAL/SY BETWEEN LAYERS OF HMA PAVEMENT.
- HMA PAVEMENT WEIGHT CALCULATIONS ARE BASED ON 112 LB/SY/IN.
- 5.5" HMA PAVEMENT SHALL BE PLACED IN TWO LAYERS FOR URBAN SECTION, A 2.5" UPPER LAYER AND A 3" LOWER LAYER.
- 4" HMA PAVEMENT SHALL BE PLACED IN TWO LAYERS FOR RURAL SECTION, A 1.75" UPPER LAYER AND A 2.25" LOWER LAYER.

STANDARD ABBREVIATIONS

AC	ACRE	IE	INVERT ELEVATION
AGG	AGGREGATE	INV	INVERT
AH	AHEAD	JCT	JUNCTION
AADT	ANNUAL AVERAGE DAILY TRAFFIC	LT	LEFT
ASPH	ASPHALTIC	L	LENGTH OF CURVE
AVG	AVERAGE	LIN FT or LF	LINEAR FOOT
BK	BACK	LS	LUMP SUM
BAD	BASE AGGREGATE DENSE	NC	NORMAL CROWN
BM	BENCH MARK	NGS	NATIONAL GEODETIC SURVEY
BR	BRIDGE	N	NORTH
CL or C/L	CENTER LINE	NB	NORTHBOUND
CE	COMMERCIAL ENTRANCE	NO	NUMBER
CONC	CONCRETE	PT	POINT
CO	COUNTY	PC	POINT OF CURVATURE
CTH	COUNTY TRUNK HIGHWAY	PI	POINT OF INTERSECTION
CR	CREEK	PT	POINT OF TANGENCY
CABC	CRUSHED AGGREGATE BASE COURSE	PCC	PORTLAND CEMENT CONCRETE
CSD	COMMUNITY SENSITIVE DESIGN	LB	POUND
CWT	HUNDREDWEIGHT	PE	PRIVATE ENTRANCE
CY or CUYD	CUBIC YARD	R	RADIUS
CULV	CULVERT	RL or R/L	REFERENCE LINE
CP	CULVERT PIPE	RT	RIGHT
C & G	CURB AND GUTTER	R/W	RIGHT-OF-WAY
D	DEGREE OF CURVE	RD	ROAD
DIA	DIAMETER	SHLDR	SHOULDER
DISCH	DISCHARGE	SB	SOUTHBOUND
E	EAST	SF or SQ FT	SQUARE FEET
EB	EASTBOUND	SY or SQ YD	SQUARE YARD
EL or ELEV	ELEVATION	SDD	STANDARD DETAIL DRAWINGS
EW	ENDWALL	STH	STATE TRUNK HIGHWAYS
ENT	ENTRANCE	SE	SUPERELEVATION
EXC	EXCAVATION	T	TANGENT
EX	EXISTING	TEMP	TEMPORARY
FERT	FERTILIZER	TWLTL	TWO-WAY LEFT-TURN LANE
FE	FIELD ENTRANCE	UG	UNDERGROUND
FL or F/L	FLOW LINE	USH	UNITED STATES HIGHWAY
FT	FOOT	V	VELOCITY OR DESIGN SPEED
HE	HIGHWAY EASEMENT	VC	VERTICAL CURVE
HMA	HOT MIX ASPHALT	WB	WESTBOUND
INL	INLET	YD	YARD

AREA CONTACTS

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CEGGERT@MEDIACOMCC.COM

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ROGER HERRIED
P.O. BOX 371
NECEDAH, WI 54646-0371
(608) 565-2261
NECEDAHADMIN@NECEDAH.US

OAKDALE ELECTRIC COOPERATIVE - ELECTRICITY
MATT RIGGS
P.O. BOX 128
OAKDALE, WI 54649-0128
(608) 372-8828
MRIGGS@OAKDALEREC.COM

TDS TELECOM - COMMUNICATION LINE
JEFF SHAW
202 E OGDEN STREET
MEDFORD, WI 54551
(715) 748-6970
JEFF.SHAW@TDSTELECOM.COM

RAILROAD CONTACT

CANADIAN NATIONAL RAILWAY
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SUPERIOR, WI 54880
(715) 345-2503
JACKIE.SAPP@CN.CA

DIGGERS HOTLINE
Dial 811 or (800) 242-8511
www.DiggersHotline.com

RUNOFF COEFFICIENT TABLE

A	HYDROLOGIC SOIL GROUP											
	B		C						D			
	SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)		
LAND USE:	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER
ROW CROPS	.08 .22	.16 .30	.22 .38	.12 .26	.20 .34	.27 .44	.15 .30	.24 .37	.33 .50	.19 .34	.28 .41	.38 .56
MEDIAN STRIP-TURF	.19 .24	.20 .26	.24 .30	.19 .25	.22 .28	.26 .33	.20 .26	.23 .30	.30 .37	.20 .27	.25 .32	.30 .40
SIDE SLOPE: TURF			.25 .32			.27 .34			.28 .36			.30 .38
PAVEMENT:												
ASPHALT	.70 - .95											
CONCRETE	.80 - .95											
BRICK	.70 - .80											
DRIVES, WALKS	.75 - .85											
ROOFS	.75 - .95											
GRAVEL ROADS, SHOULDERS	.40 - .60											

TOTAL PROJECT AREA = 561 ACRES
TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = 2.21 ACRES

ORDER OF TYPICAL SECTION & DETAIL SHEETS

1. GENERAL NOTES
2. PROJECT OVERVIEW
3. TYPICAL SECTIONS
4. CONSTRUCTION DETAILS
5. REMOVAL PLAN
6. CURB RAMP DETAILS
7. TRAFFIC CONTROL PLAN
8. DETOUR PLAN
9. ALIGNMENT DETAIL

2

SUPERELEVATION DATA STH 80 CURVE NO. 1

					IN			OUT			30 MPH
	CURVE NO.	PC	PT	D	NORMAL SECTION	2% SUPER	FULL SUPER	FULL SUPER	2% SUPER	NORMAL SECTION	SUPER RATE (%)
EXISTING	1										N.C.
PROPOSED	1	227+43.70	230+10.82	10°51'41"	226+49.45	227+21.95	227+72.70	229+81.83	230+32.58	231+05.08	N.C.

SUPERELEVATION DATA STH 80 CURVE NO. 2

					IN			OUT			30 MPH
	CURVE NO.	PC	PT	D	NORMAL SECTION	2% SUPER	FULL SUPER	FULL SUPER	2% SUPER	NORMAL SECTION	SUPER RATE (%)
EXISTING	2										5.0
PROPOSED	2	232+02.55	234+77.11	10°27'56"	-	231+80.80'	232+31.55'	234+48.11'	234+98.86'	235+71.36'	5.0

SUPERELEVATION DATA STH 80 CURVE NO. 3

					IN			OUT			30 MPH
	CURVE NO.	PC	PT	D	NORMAL SECTION	2% SUPER	FULL SUPER	FULL SUPER	2% SUPER	NORMAL SECTION	SUPER RATE (%)
EXISTING	3										6.0
PROPOSED	3	238+54.63	241+19.28	20°47'28"	237+45.63	238+18.30	238+90.96	240+82.95	241+55.62	242+28.29	6.0

SUPERELEVATION DATA STH 80 CURVE NO. 4

					IN			OUT			40 MPH
	CURVE NO.	PC	PT	D	NORMAL SECTION	2% SUPER	FULL SUPER	FULL SUPER	2% SUPER	NORMAL SECTION	SUPER RATE (%)
EXISTING	4										4.5
PROPOSED	4	257+82.38	266+03.28	6°49'05"	0	6	6	0	0	0	4.5

SUPERELEVATION DATA STH 80 CURVE NO. 5

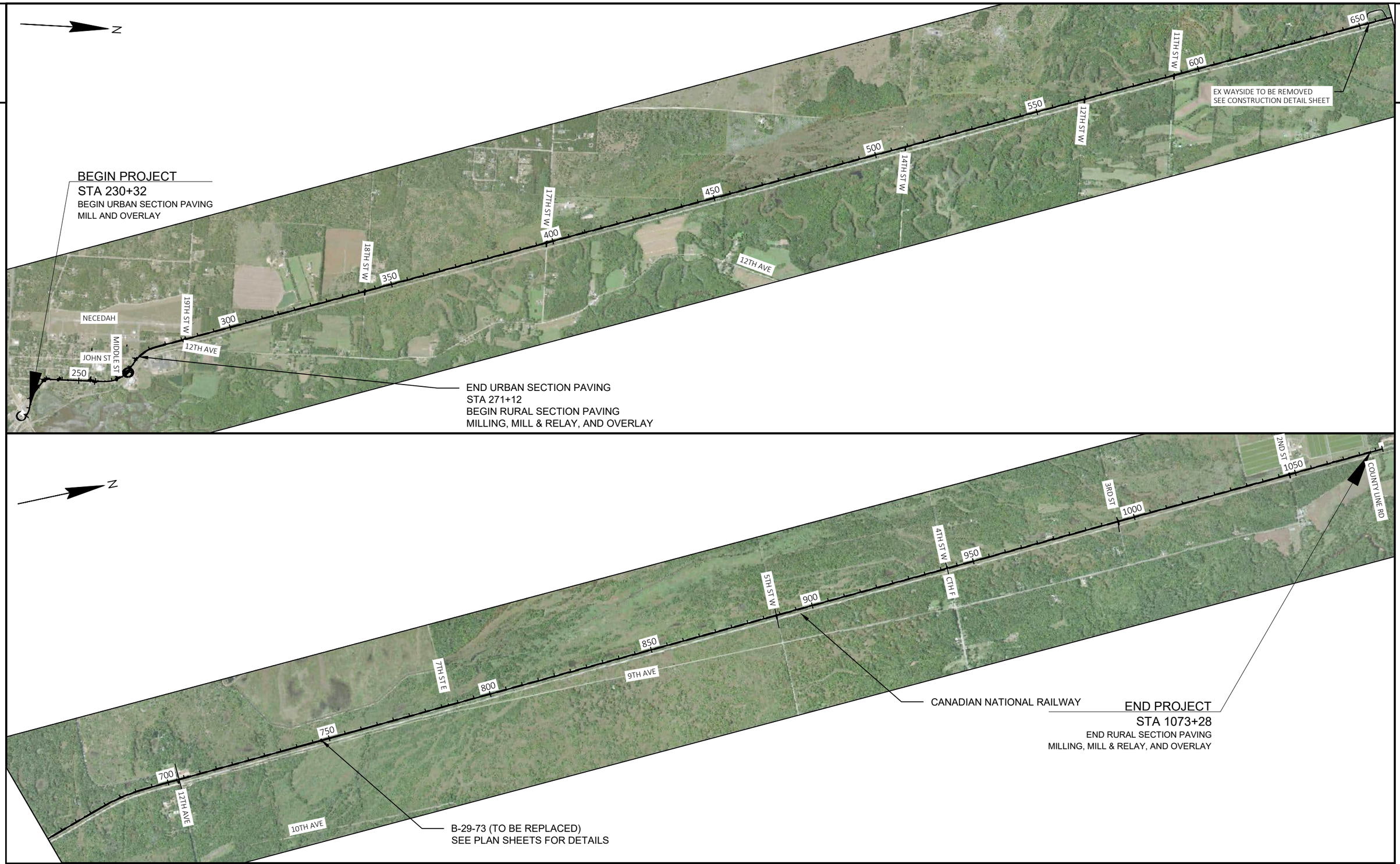
					IN			OUT			50 MPH
	CURVE NO.	PC	PT	D	NORMAL SECTION	2% SUPER	FULL SUPER	FULL SUPER	2% SUPER	NORMAL SECTION	SUPER RATE (%)
EXISTING	5										5.5
PROPOSED	5	269+27.67	277+02.49	5°00'14"	-	268+82.93	269+73.99	276+56.16	277+47.23	278+43.10	5.5

SUPERELEVATION DATA STH 80 CURVE NO. 6

					IN			OUT			60 MPH
	CURVE NO.	PC	PT	D	NORMAL SECTION	2% SUPER	FULL SUPER	FULL SUPER	2% SUPER	NORMAL SECTION	SUPER RATE (%)
EXISTING	6										2.7
PROPOSED	6	679+14.50	692+46.27	1°04'14"	678+07.83	679+14.50	679+41.16	692+19.61	692+46.28	693+52.94	**2.9**

BORING SUMMARY

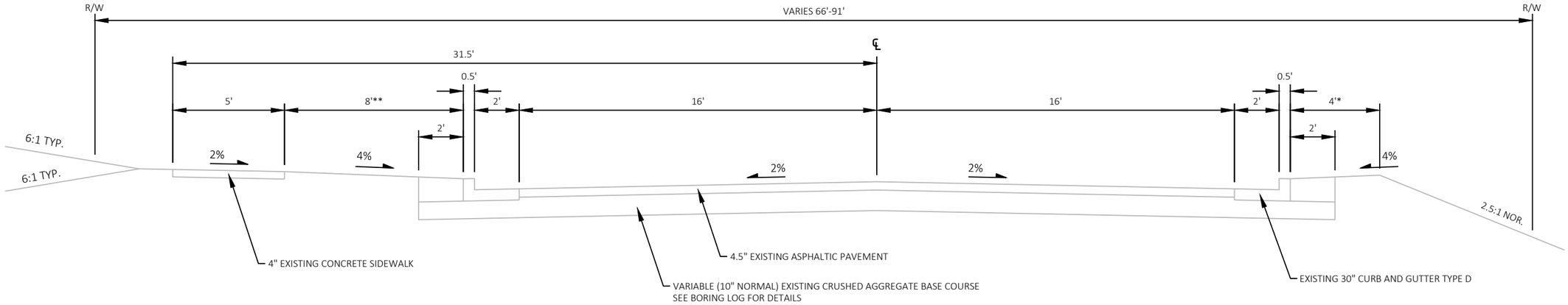
BORING NUMBER	LOG MILE	STATION	OFFSET	EXISTING ASPAHLT DEPTH	BASE AGGREGATE DEPTH	PAVEMENT STRUCTURE DEPTH	SUBGRADE TYPE
B-1	0.04	229+54	15' RT	11" PCC	8"	19"	SAND
B-2	0.31	243+80	6' LT	4"	10"	14"	SAND
B-3	0.56	257+00	9' LT	4.5"	10"	14.5"	SAND
B-4	0.86	272+84	6' RT	4"	19"	23"	SAND
B-5	1.08	284+45	9' RT	7"	9"	16"	SAND
B-6	1.34	298+18	6' RT	7"	10"	17"	SAND
B-7	1.62	312+97	12' RT	6"	13"	19"	SAND
B-8	1.89	327+22	9' LT	6"	11"	17"	SAND
B-9	2.21	344+12	6' LT	7"	10"	17"	SAND
B-10	2.45	356+79	12' LT	7"	10"	17"	SAND
B-11	2.72	371+05	6' LT	6"	8"	14"	SAND
B-12	2.99	385+30	15' LT	4"	11"	15"	SAND
B-13	3.27	400+09	9' RT	6.5"	9"	15.5"	SAND
B-14	3.50	412+23	6' RT	6.5"	7.5"	14"	SAND
B-15	3.75	425+43	12' RT	7.5"	6.5"	14"	SAND
B-16	4.00	438+63	6' RT	8"	9"	17"	SAND
B-17	4.27	452+89	15' RT	3"	12"	15"	SAND
B-18	4.51	465+56	9' LT	7.5"	8.5"	16"	SAND
B-19	4.77	479+29	6' LT	7"	7"	14"	SAND
B-20	5.01	491+96	12' LT	4"	4" RAP/7" BAD	15"	SAND
B-21	5.27	505+69	6' LT	7.5"	3" RAP/4.5" BAD	15"	SAND
B-22	5.5	517+83	15' LT	4"	15"	19"	SAND
B-23	5.74	530+50	9' RT	8"	9"	17"	SAND
B-24	5.97	542+65	6' RT	9.5"	7.5"	17"	SAND
B-25	6.26	557+96	12' RT	9.5"	8"	17.5"	SAND
B-26	6.49	570+10	6' RT	6"	5.5"	11.5"	SILTY SAND
B-27	6.74	583+30	15' RT	2.5"	9.5"	12"	SILTY SAND
B-28	7.00	597+03	9' LT	6.5"	7.5"	14"	SAND
B-29	7.25	610+23	6' LT	7"	5"	12"	SILTY SAND
B-30	7.49	622+90	12' LT	3.5"	6" RAP/6" BAD	15.5"	SAND
B-31	7.75	636+63	6' LT	7.5"	4"	11.5"	SAND
B-32	8.04	651+94	18' RT	6"	7"	13"	SAND
B-33	8.30	665+67	9' RT	7.5"	4"	11.5"	SAND
B-34	8.55	678+87	6' RT	7.5"	6.5"	14"	SAND
B-35	8.76	689+96	12' RT	3.5"	4.5" RAP/6" BAD	14"	SAND
B-36	9.00	702+63	6' RT	6.5"	7.5"	14"	SAND
B-37	9.25	715+83	15' LT	2.5"	9.5"	12"	SAND
B-38	9.51	729+56	9' LT	7"	6"	13"	SAND
B-39	9.77	743+29	6' LT	7.5"	6.5"	14"	SAND
B-40	10.00	755+43	12' LT	3"	4" RAP/7" BAD	14"	SAND
B-41	10.25	768+63	6' LT	7.5"	10.5"	18"	SAND
B-42	10.52	782+89	15' LT	2.5"	12.5"	15"	SAND
B-43	10.75	795+03	9' RT	6.5"	8"	14.5"	SILTY SAND
B-44	11.00	808+23	6' RT	6"	10"	16"	SILTY SAND
B-45	11.24	820+90	12' RT	6"	8"	14"	SAND
B-46	11.5	834+63	6' RT	6.5"	7.5"	14"	SAND
B-47	11.75	847+83	15' RT	3"	11"	14"	SAND
B-48	12.01	861+56	9' LT	6"	9"	15"	SAND
B-49	12.27	875+29	6' RT	7"	6"	13"	SAND
B-50	12.5	887+43	12' LT	5"	7"	12"	SAND
B-51	12.79	902+74	6' LT	7.5"	7.5"	15"	SAND
B-52	13.01	914+36	15' LT	3"	11"	14"	SAND
B-53	13.25	927+03	9' RT	7"	8"	15"	SAND
B-54	13.5	940+23	6' RT	7"	6"	13"	SILTY SAND
B-55	13.75	953+43	12' RT	6.5"	8.5"	15"	SAND
B-56	13.97	965+05	6' RT	6.5"	8.5"	15"	SAND
B-57	14.25	979+83	15' RT	3"	9"	12"	SAND
B-58	14.5	993+03	9' LT	6.5"	6.5"	13"	SAND
B-59	14.75	1006+23	6' LT	6.5"	7.5"	14"	SAND
B-60	14.99	1018+90	12' LT	6"	8"	14"	SAND
B-61	15.25	1032+63	6' LT	6"	9.5"	15.5"	SAND
B-62	15.50	1045+83	15' LT	2"	12.5"	14.5"	SAND
B-63	15.77	1060+09	9' RT	6.5"	8"	14.5"	SAND
B-64	16.00	1072+23	6' RT	7"	6"	13"	SAND



PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	PROJECT OVERVIEW	SHEET 6	E

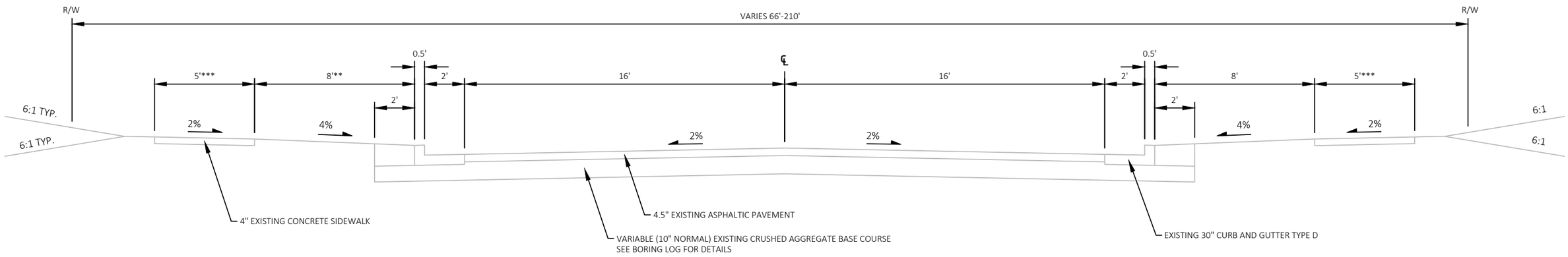
FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\020201_PO.DWG
LAYOUT NAME - Plan Dbl 1 IN 40 FT

PLOT DATE : 5/27/2022 11:08 AM PLOT BY : MARZOLF, ROBBI PLOT NAME : PLOT SCALE : 1 IN:0.5 MI



TYPICAL EXISTING SECTION - STH 80 NECEDAH

STA. 230+32 - STA. 239+50



*VARIES FROM 4.0' TO 20.5' BETWEEN STA. 230+32 TO STA. 239+21

**VARIES FROM 8.0' TO 21.8' BETWEEN STA. 239+50 TO STA. 241+00

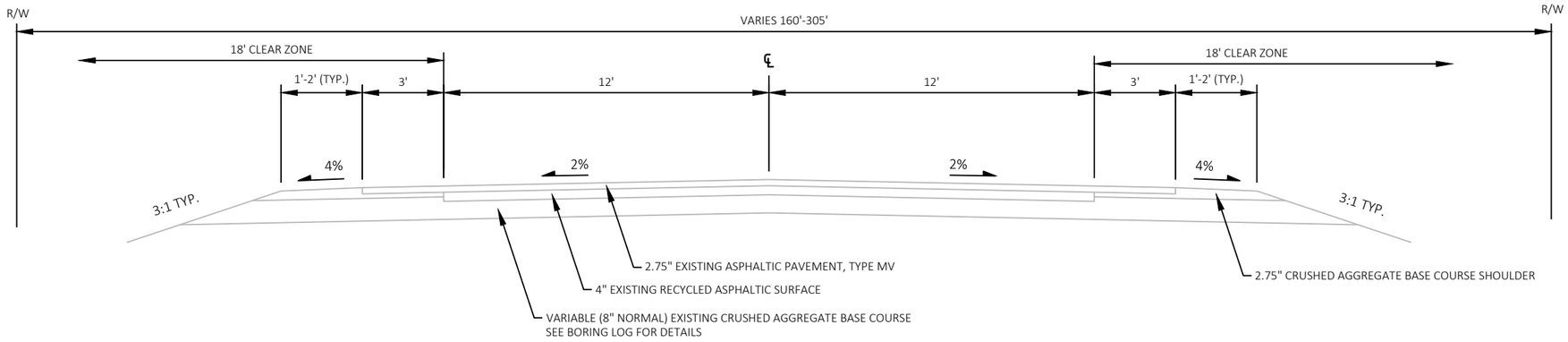
***SIDEWALK ENDS AT STA. 269+60 LT.
SIDEWALK ENDS AT STA. 265+30 RT.

TYPICAL EXISTING SECTION - STH 80 NECEDAH

STA. 239+50 - STA. 271+12

PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	TYPICAL SECTIONS	SHEET 7	E

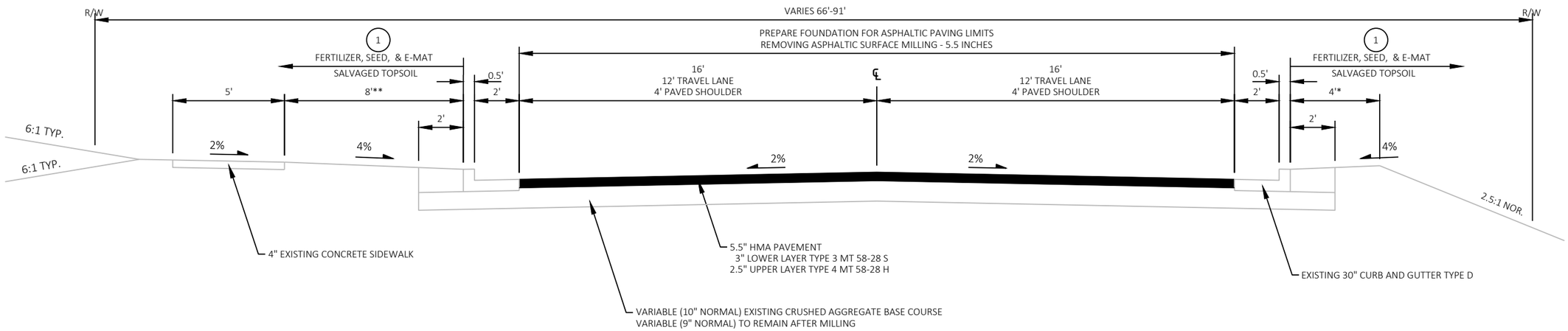
2



TYPICAL EXISTING SECTION - STH 80

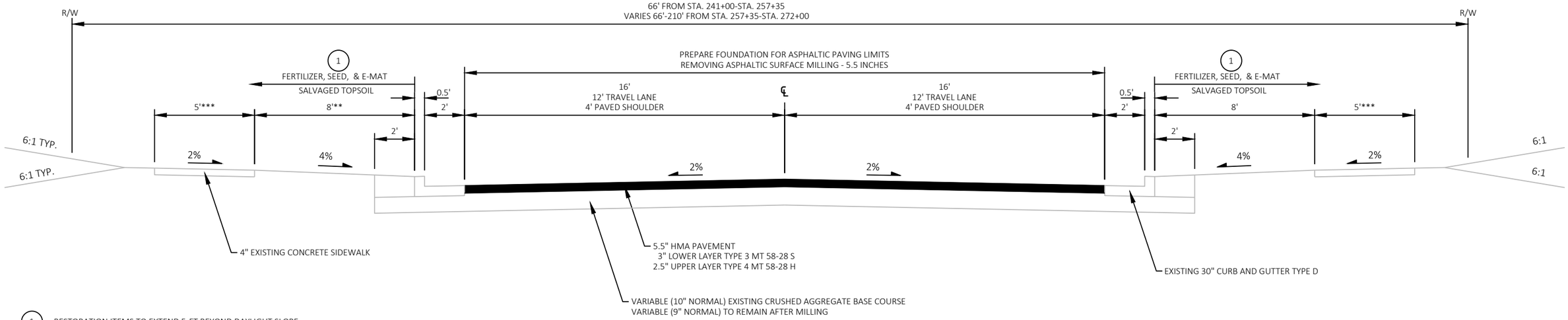
STA. 271+12 - STA 1073+28

NOTE: EXISTING ASPHALT SURFACE VARIES WITH DEPTH (SEE BORING SUMMARY)



TYPICAL PROPOSED SECTION - STH 80 NECEDAH

STA. 230+32 - STA. 239+50



TYPICAL PROPOSED SECTION - STH 80 NECEDAH

STA. 239+50 - STA. 271+12

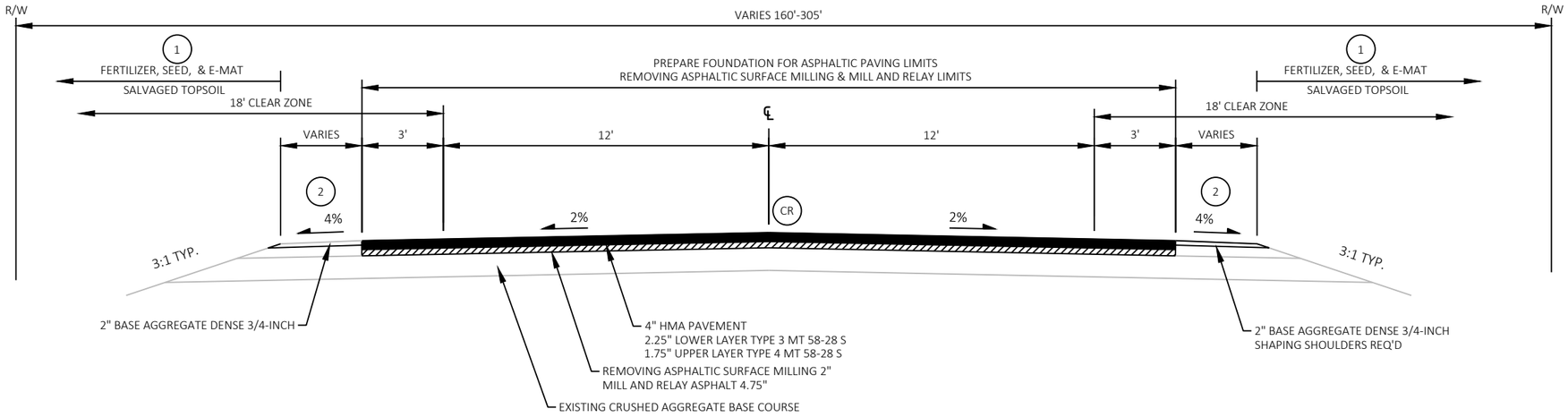
(1) RESTORATION ITEMS TO EXTEND 5-FT BEYOND DAYLIGHT SLOPE

*VARIES FROM 4.0' TO 20.5' BETWEEN STA. 230+25 TO STA. 239+21

**VARIES FROM 8.0' TO 26' BETWEEN STA. 238+45.71 TO STA. 241+00

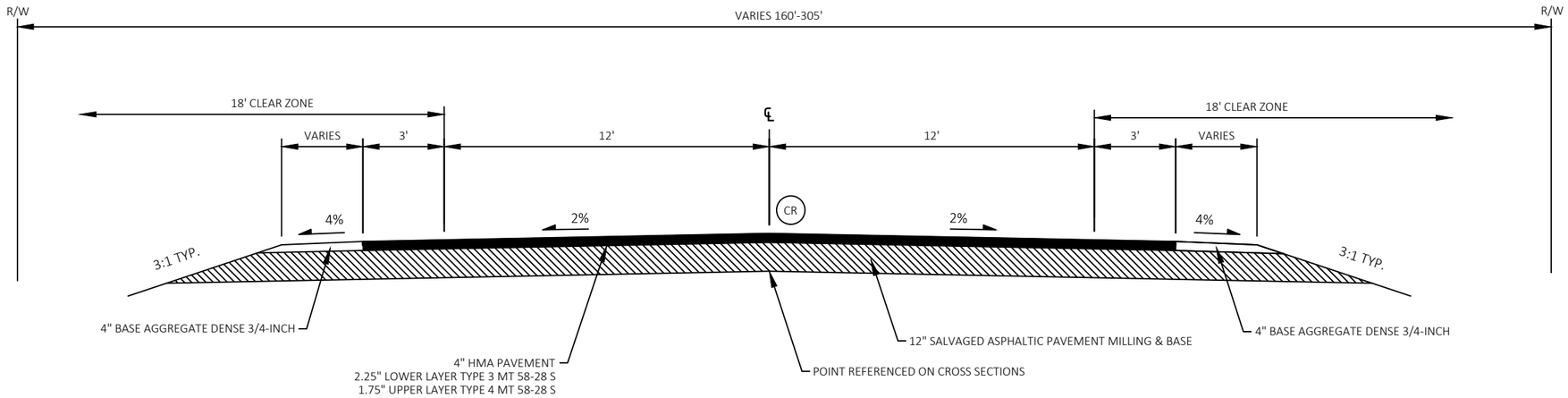
***SIDEWALK ENDS AT STA. 269+50 LT.
SIDEWALK ENDS AT STA. 265+30 RT.

PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	TYPICAL SECTIONS	SHEET 9	E



TYPICAL PROPOSED SECTION - STH 80

STA. 271+12 - STA. 747+46
STA. 747+72 - STA. 1073+28



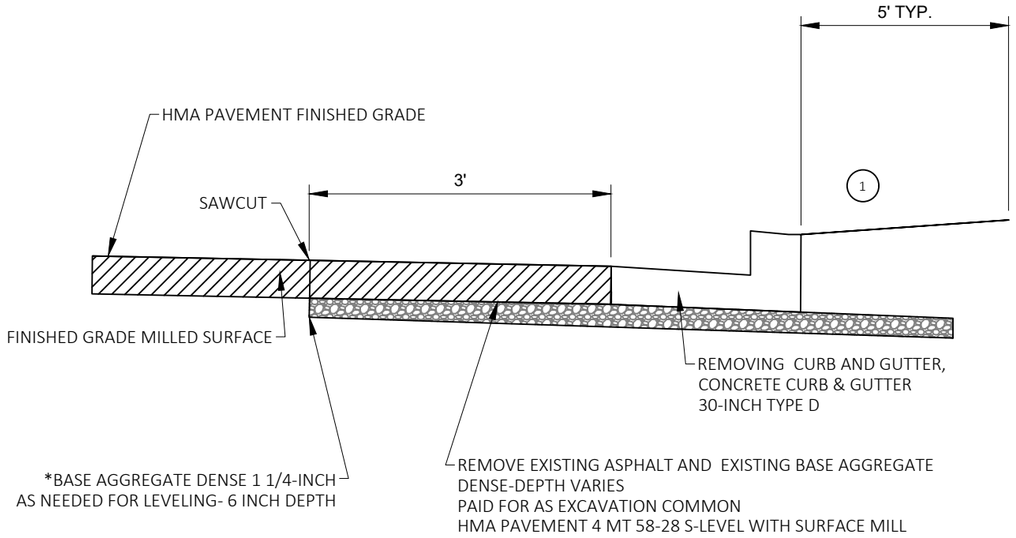
TYPICAL PROPOSED SECTION - STH 80 CULVERT

STA. 747+46 - STA. 747+72

(1) RESTORATION ITEMS TO EXTEND 5-FT BEYOND DAYLIGHT SLOPE
(2) ROUND SHOULDER TO MATCH EXISTING SLOPE
(CR) CENTERLINE RUMBLE STRIPS

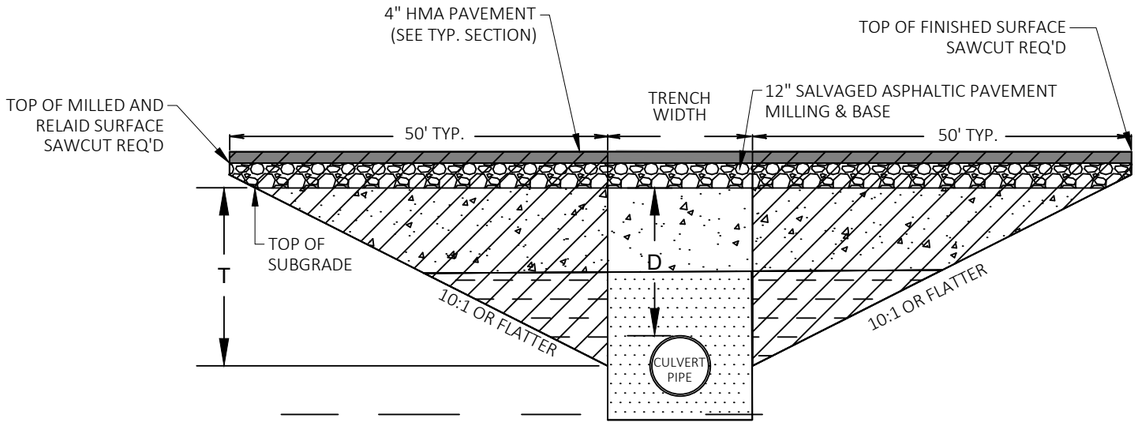
PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	TYPICAL SECTIONS	SHEET 10	E

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\020301_TS.DWG
LAYOUT NAME - 4
PLOT DATE : 5/27/2022 11:10 AM PLOT BY : MARZOLF, ROBBI PLOT NAME : PLOT SCALE : ##########



CURB & GUTTER REMOVAL AND REPLACE DETAIL

(1) TOPSOIL, EROSION MAT URBAN CLASS I TYPE A, FERTIZER TYPE B, SEEDING MIXTURE NO. 40 REQ'D



TRANSITION CUT DEPTH (T) = THE LESSER OF DEPTH TO CENTER OF PIPE OR 5 FT.
DO NOT EXTEND TRANSITION CUT BELOW HORIZONTAL CENTER OF PIPE.

CULVERT INSTALLATION DETAIL

DEPTH D < 6 FT

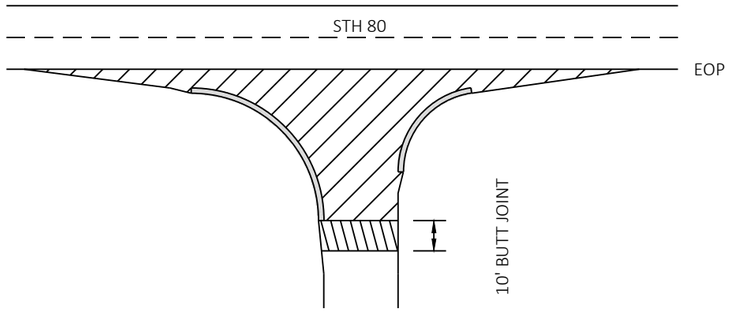
NOTES

TRANSITION CUT IS PAID AS EXCAVATION COMMON.
TRANSITION CUT WIDTH IS FROM SUBGRADE SHOULDER POINT TO SUBGRADE SHOULDER POINT.
BACKFILL THE TRANSITION CUT AREAS WITH FOUNDATION AND TRENCH BACKFILL AS SPECIFIED IN STANDARD SPEC 520.
PERFORM CULVERT PIPE INSTALLATION BEFORE MILL AND RELAY.
PLACE 4"ASPHALT PAVEMENT WITH FINAL PAVING OPERATION.

CULVERT PIPE TRANSITION

ROUTE	STA.	DEPTH	PIPE DIA. (IN)	REMARKS
STH 80	327+56	X.X FT	24	REPLACE IN KIND
STH 80	500+38	X.X FT	24	REPLACE IN KIND
STH 80	747+49	0.8 FT	43 X 68	B-29-159
STH 80	747+59	0.8 FT	43 X 68	B-29-159
STH 80	747+69	0.8 FT	43 X 68	B-29-159
STH 80	1068+17	X.X FT	24	REPLACE IN KIND

PROPOSED SURFACE
PROPOSED BASE
TRENCH BACKFILL
TRENCH OR FOUNDATION BACKFILL
FOUNDATION BACKFILL
TRANSITION CUT

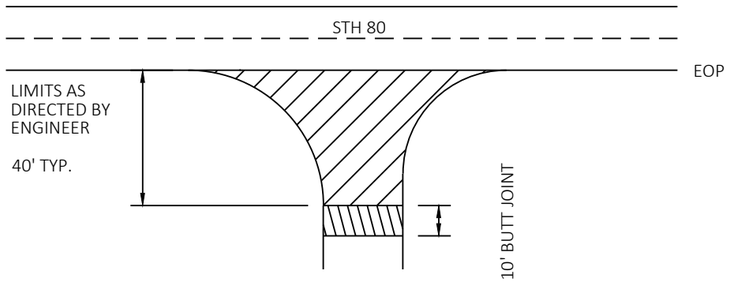


REMOVING ASPHALTIC SURFACE MILLING

REMOVING ASPHALTIC SURFACE BUTT JOINTS
SEE BUTT JOINT DETAIL

NOTE: WHEN MATCHING TO AN UNPAVED SURFACE BUTT JOINT IS NOT REQUIRED

SIDE ROADS
WITH CURB AND GUTTER

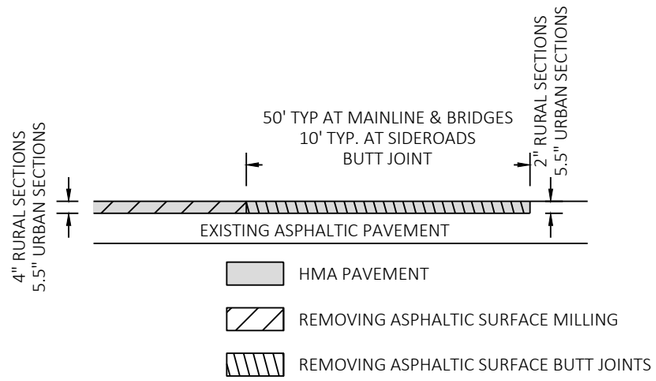


REMOVING ASPHALTIC SURFACE MILLING

REMOVING ASPHALTIC SURFACE BUTT JOINTS
SEE BUTT JOINT DETAIL

NOTE: WHEN MATCHING TO AN UNPAVED SURFACE BUTT JOINT IS NOT REQUIRED

SIDE ROADS
WITHOUT CURB AND GUTTER



BUTT JOINT
MAINLINE AND BRIDGES
SIDE ROADS

NOTE: SAWCUTS IN THESE LOCATIONS ARE INCIDENTAL TO REMOVING ASPHALTIC SURFACE BUTT JOINTS ITEM

2

2

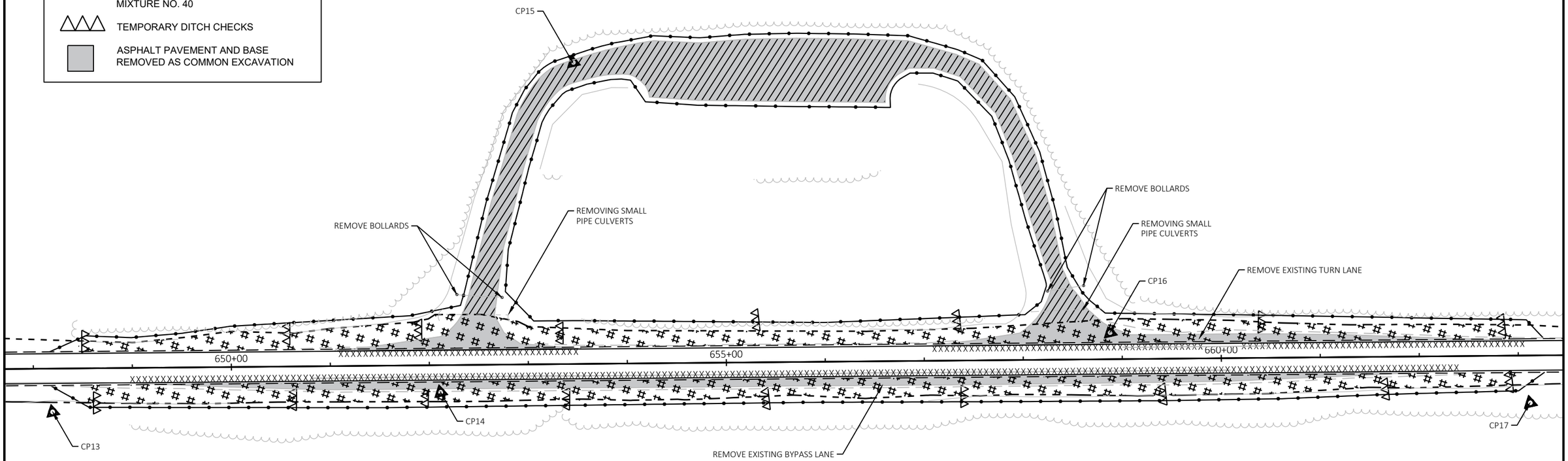
LEGEND

- XXXXXXXX SAWCUT
- EXCAVATION COMMON, TOPSOIL, FERTILIZER TYPE B, SEEDING MIXTURE NO. 40, MULCHING
- SILT FENCE
- EXCAVATION COMMON, EROSION MAT URBAN CLASS 1 TYPE A, SALVAGED TOPSOIL, FERTILIZER TYPE B, SEEDING MIXTURE NO. 40
- TEMPORARY DITCH CHECKS
- ASPHALT PAVEMENT AND BASE REMOVED AS COMMON EXCAVATION

N

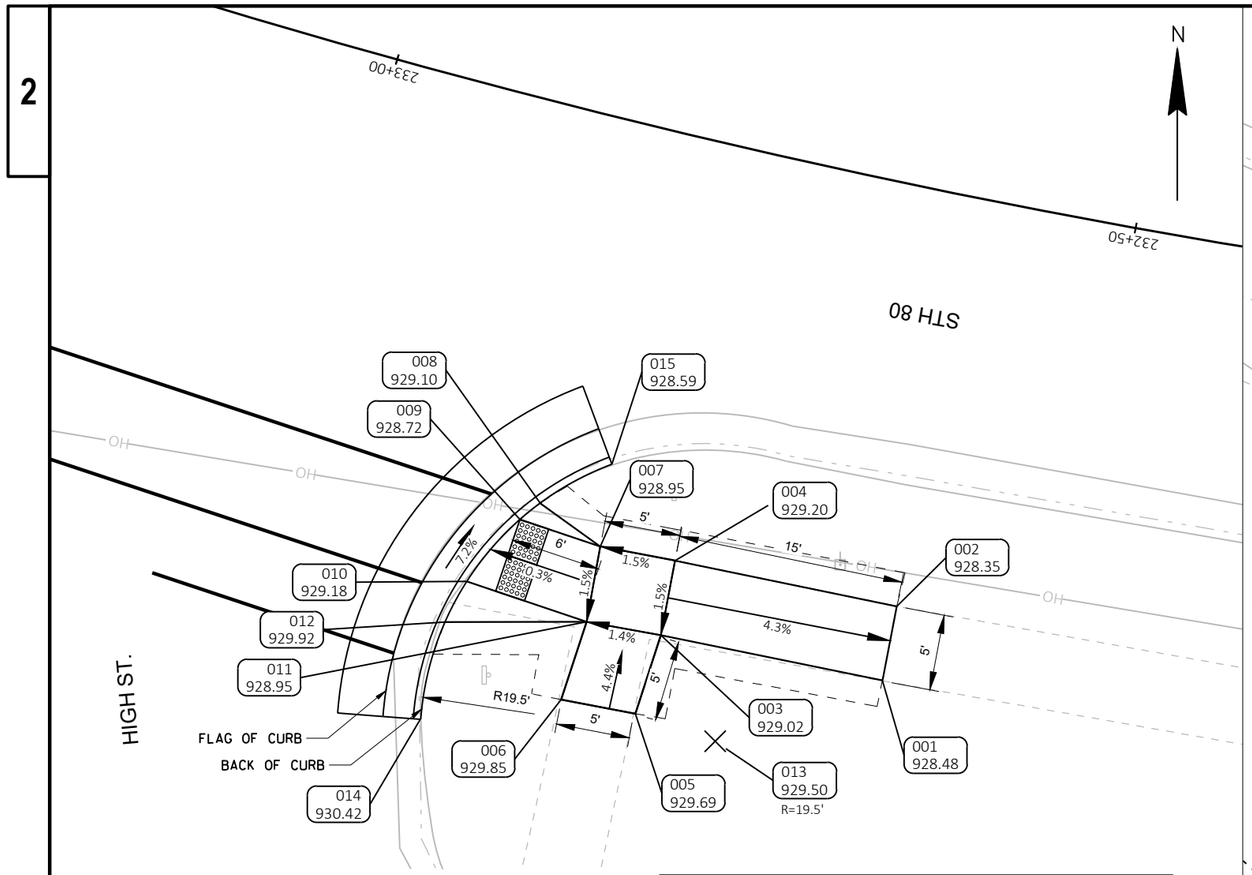
NOTES:

1. REMOVE WAYSIDE ASPHALT PAVEMENT AND BASE COURSE UNTIL SUBGRADE MATERIAL IS REACHED.
2. PERPETUATE DITCH ACROSS OLD WAYSIDE ENTRANCES. SEE CROSS SECTIONS FOR DETAILS.



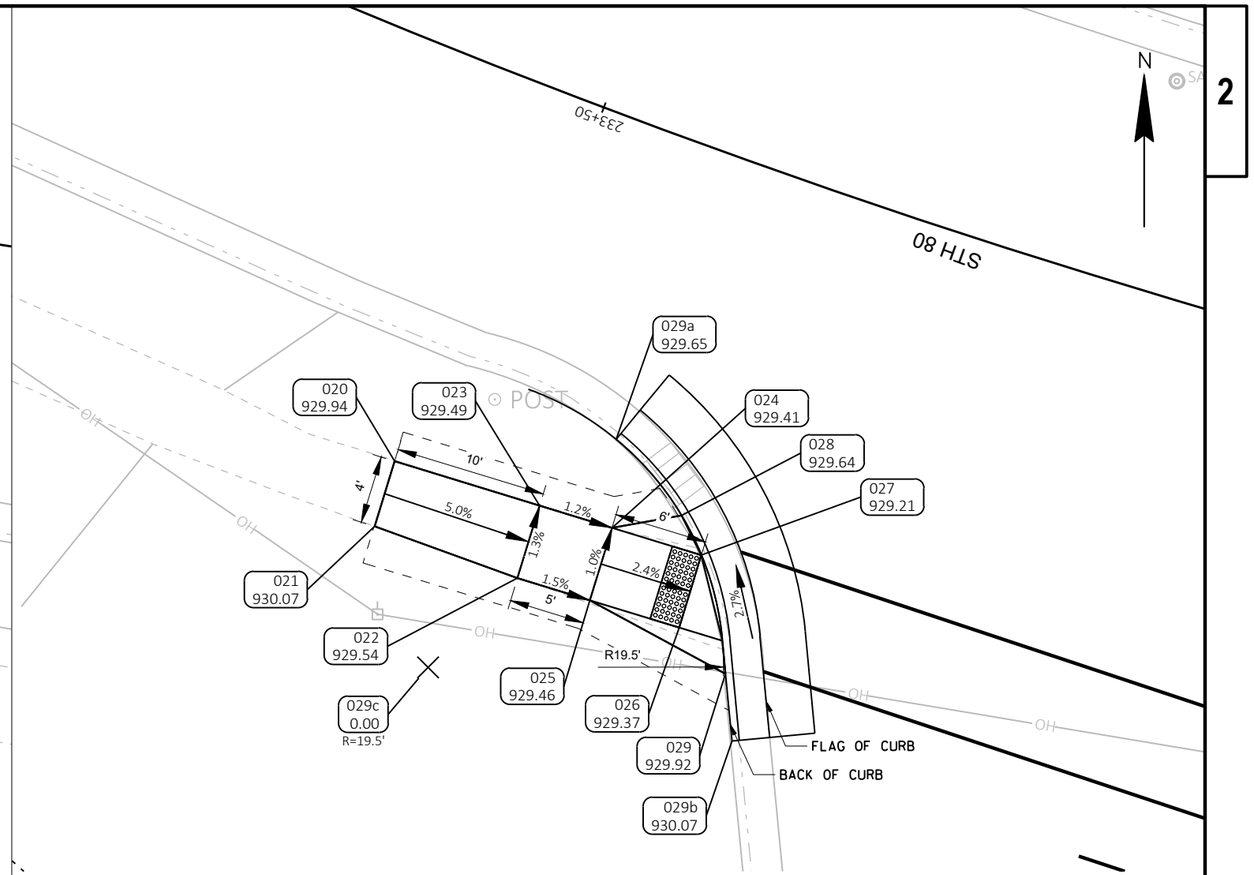
SCALE, FEET 0 50 100

PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	REMOVAL PLAN - WAYSIDE	SHEET 13	E



STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
001	232+60.51	-32.44 LT	240524.08	463075.22	928.48
002	232+60.55	-27.46 LT	240528.97	463076.17	928.35
003	232+74.60	-32.58 LT	240527.07	463060.60	929.02
004	232+74.77	-27.59 LT	240531.98	463061.54	929.20
005	232+75.06	-38.01 LT	240521.89	463058.92	929.69
006	232+79.72	-38.22 LT	240522.81	463054.02	929.85
007	232+79.55	-27.78 LT	240532.92	463056.60	928.95
008	232+83.87	-25.94 LT	240535.78	463052.63	929.10
009	232+84.86	-27.30 LT	240534.70	463051.30	928.72
010	232+87.15	-32.12 LT	240530.62	463047.78	929.18
011	232+79.30	-32.83 LT	240527.95	463055.70	928.95
012	232+87.85	-35.07 LT	240527.94	463046.34	929.92
013	232+69.89	-38.63 LT	240520.07	463064.19	929.50
014	232+87.84	-41.67 LT	240521.53	463044.74	930.42
015	232+80.03	-22.34 LT	240538.34	463057.37	928.59

HIGH ST. & STH 80
SE QUADRANT
CURB RAMP TYPE 4B1-PAVED FLARES

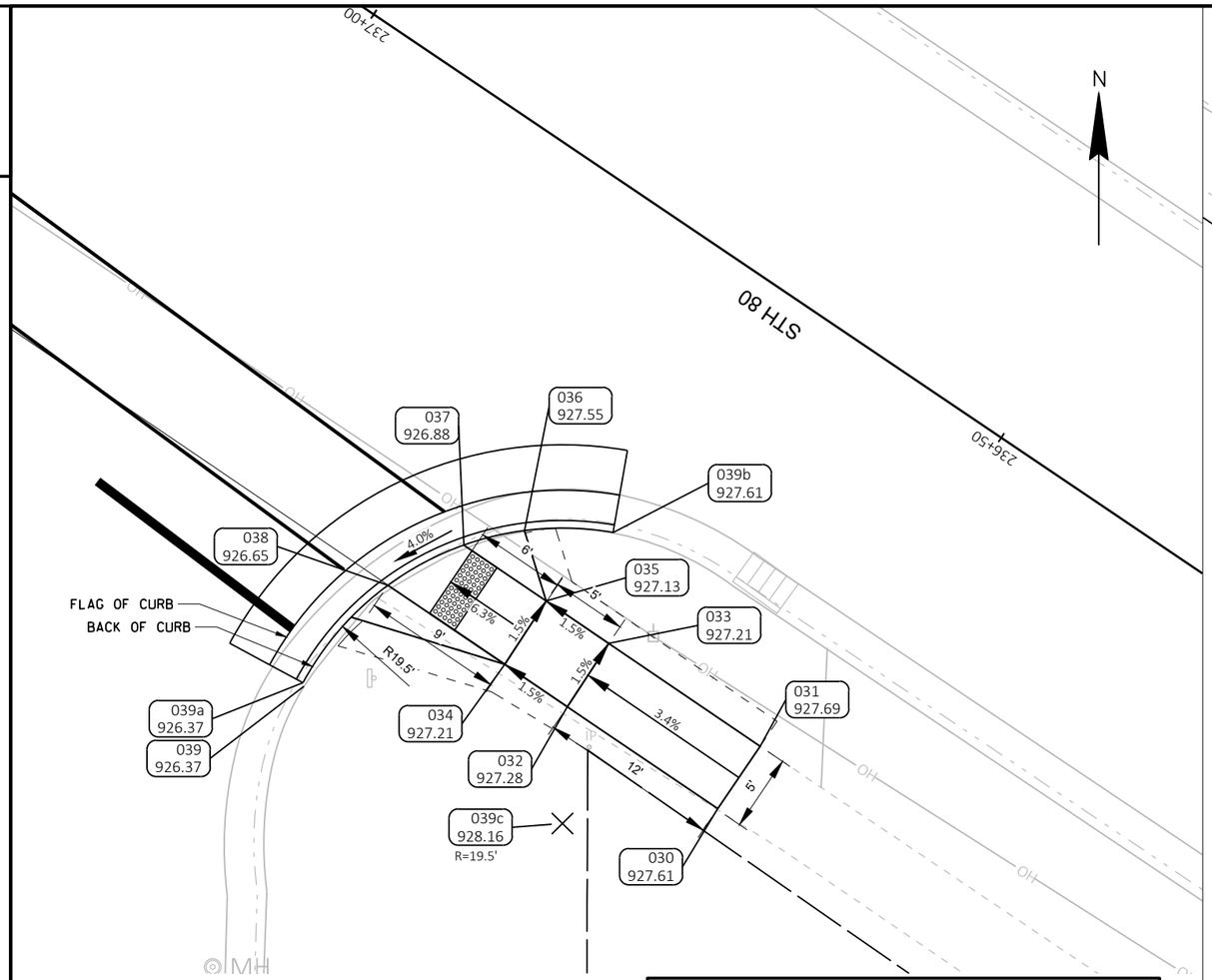


STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
020	233+54.44	-26.72 LT	240557.23	462981.89	929.94
021	233+54.13	-31.20 LT	240552.93	462980.58	930.07
022	233+44.66	-31.08 LT	240549.52	462989.99	929.54
023	233+44.92	-26.08 LT	240554.30	462991.46	929.49
024	233+40.15	-25.83 LT	240552.84	462996.24	929.41
025	233+39.93	-30.82 LT	240548.06	462994.77	929.46
026	233+34.11	-30.57 LT	240546.26	463000.66	929.37
027	233+34.28	-25.58 LT	240551.04	463002.12	929.21
028	233+36.28	-23.54 LT	240553.65	463000.81	929.64
029	233+30.44	-32.47 LT	240543.21	463003.72	929.92
029C	233+47.94	-38.66 LT	240543.62	462984.09	0.00
029B	233+28.73	-36.55 LT	240538.77	463004.14	930.07
029A	233+41.80	-20.26 LT	240558.67	462996.50	929.65

HIGH ST. & STH 80
SW QUADRANT
CURB RAMP TYPE 4B1-PAVED FLARES

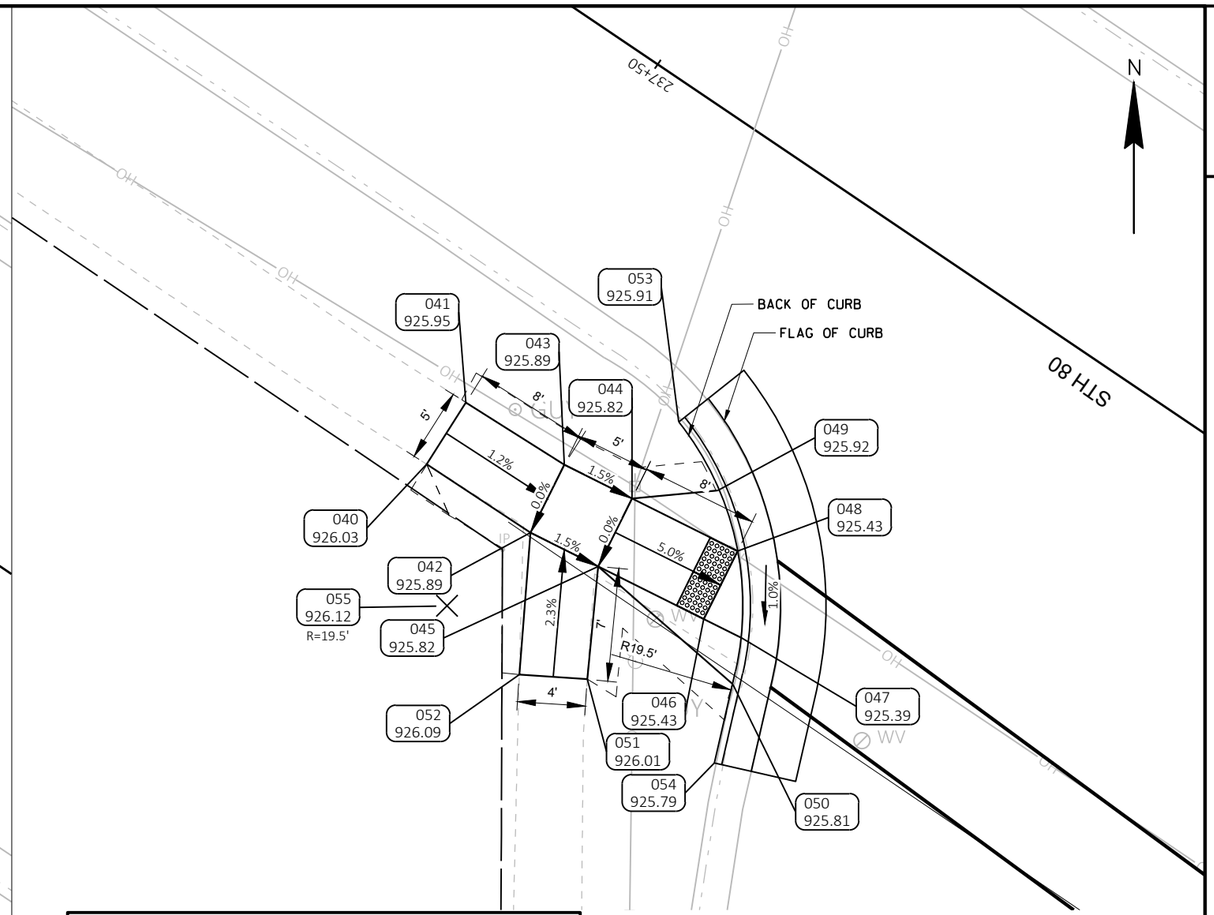
SCALE, FEET 0 5 10

PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	CURB RAMP DETAILS	SHEET 14	E



STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
030	236+51.82	-30.79 LT	240711.03	462721.00	927.61
031	236+51.81	-25.80 LT	240715.16	462723.80	927.69
032	236+63.82	-30.76 LT	240717.76	462711.06	927.28
033	236+63.89	-25.76 LT	240721.94	462713.80	927.21
034	236+68.82	-30.75 LT	240720.56	462706.92	927.21
035	236+68.89	-25.75 LT	240724.75	462709.66	927.13
036	236+72.67	-22.71 LT	240729.39	462708.23	927.55
037	236+75.45	-25.73 LT	240728.43	462704.23	926.88
038	236+78.13	-30.73 LT	240725.79	462699.22	926.65
039	236+79.00	-39.37 LT	240719.11	462693.67	926.37
039A	236+79.19	-39.22 LT	240719.34	462693.59	926.37
039B	236+67.75	-19.54 LT	240729.26	462714.08	927.61
039C	236+59.78	-37.34 LT	240710.05	462710.73	928.16

N DIVISION ST. & STH 80
SE QUADRANT
CURB RAMP TYPE 4B1-PAVED FLARES



STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
040	237+47.88	-30.40 LT	240765.08	462641.59	926.03
041	237+48.02	-25.57 LT	240769.16	462644.17	925.95
042	237+39.69	-30.30 LT	240760.57	462648.43	925.89
043	237+40.34	-25.35 LT	240765.05	462650.66	925.89
044	237+35.38	-24.69 LT	240762.81	462655.13	925.82
045	237+34.73	-29.65 LT	240758.34	462652.90	925.82
046	237+27.02	-28.63 LT	240754.87	462659.86	925.43
047	237+24.31	-28.28 LT	240753.65	462662.31	925.39
048	237+27.68	-23.68 LT	240759.35	462662.09	925.43
049	237+30.98	-21.09 LT	240763.34	462660.80	925.92
050	237+23.03	-31.13 LT	240750.56	462661.78	925.81
051	237+31.17	-36.23 LT	240750.90	462652.17	926.01

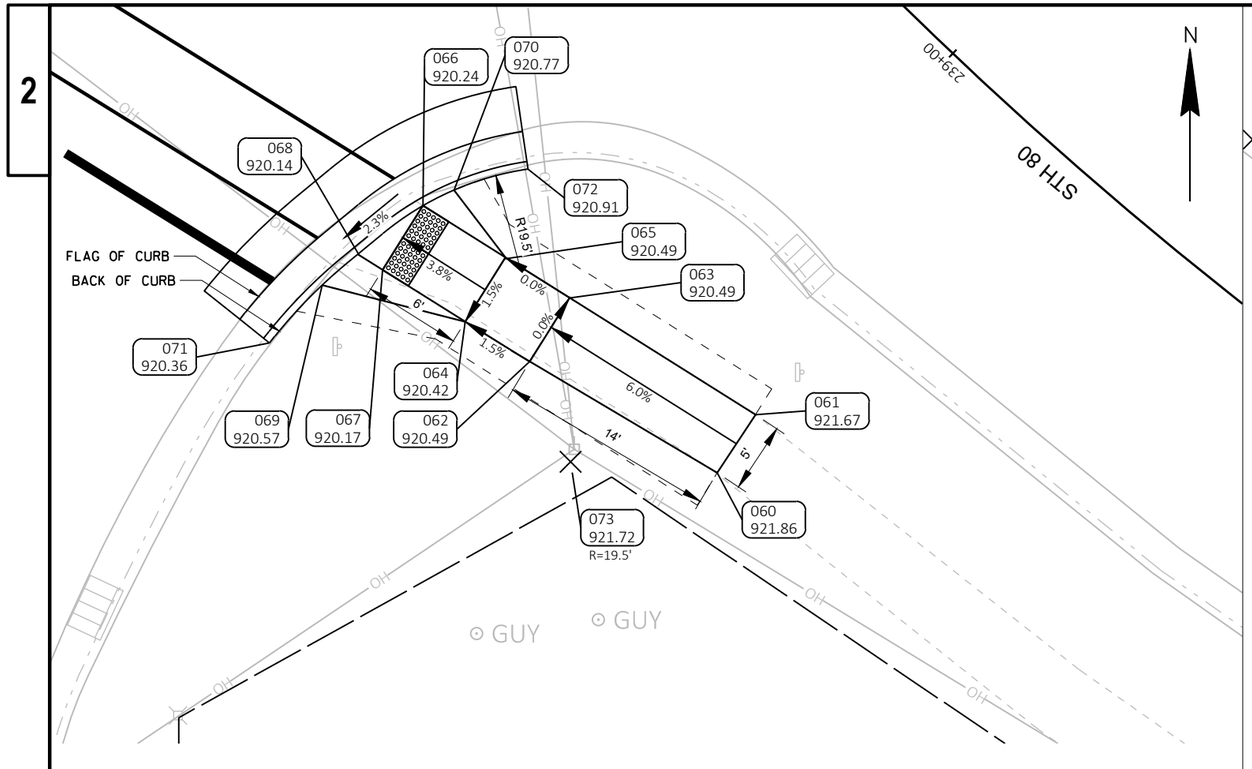
STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
052	237+35.06	-38.47 LT	240751.21	462647.69	926.09
053	237+35.66	-18.79 LT	240767.86	462658.20	925.91
054	237+21.13	-36.11 LT	240745.37	462660.56	925.79
055	237+41.55	-37.38 LT	240755.74	462642.93	926.12

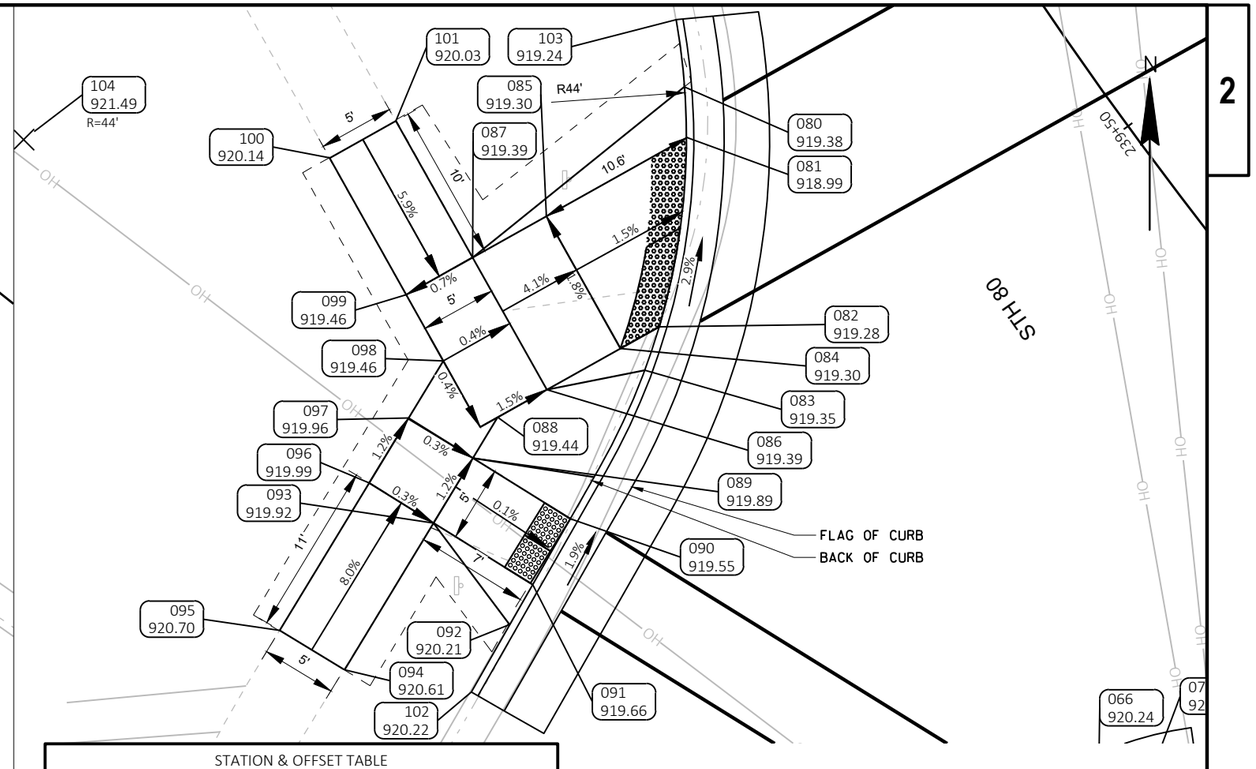
N DIVISION ST. & STH 80
SW QUADRANT
CURB RAMP TYPE 4B1-PAVED FLARES

SCALE, FEET 0 5 10

PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	CURB RAMP DETAILS	SHEET 15	E

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\021003_CD.DWG
LAYOUT NAME - 2
PLOT DATE : 7/5/2022 5:35 PM
PLOT BY : MARZOLF, ROBBI
PLOT NAME :
PLOT SCALE : ##########
WISDOT/CADDS SHEET 42





STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
060	238+93.05	-30.87 LT	240850.67	462519.18	921.86
061	238+93.68	-26.33 LT	240854.50	462521.71	921.67
062	239+05.61	-34.02 LT	240858.00	462506.81	920.49
063	239+06.58	-29.19 LT	240862.19	462509.44	920.49
064	239+09.93	-35.19 LT	240860.61	462502.55	920.42
065	239+10.95	-30.41 LT	240864.78	462505.16	920.49
066	239+16.58	-31.89 LT	240868.29	462499.77	920.24
067	239+15.45	-36.74 LT	240864.04	462497.10	920.17
068	239+17.02	-37.19 LT	240865.03	462495.56	920.14
069	239+17.21	-40.32 LT	240863.05	462493.12	920.57
070	239+15.99	-29.69 LT	240869.31	462501.82	920.77
071	239+16.85	-45.47 LT	240859.24	462489.64	920.36
072	239+13.85	-25.20 LT	240870.69	462506.70	920.91
073	238+99.82	-36.90 LT	240851.39	462509.51	921.72

PLUM ST. & STH 80
SE QUADRANT
CURB RAMP TYPE 4B1-PAVED FLARES

STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
080	239+67.90	-22.73 LT	240918.39	462472.46	919.38
081	239+65.25	-24.41 LT	240915.06	462472.61	918.99
082	239+56.76	-32.86 LT	240902.57	462470.81	919.28
083	239+55.16	-35.29 LT	240899.71	462469.86	919.35
084	239+57.01	-35.79 LT	240901.14	462468.25	919.30
085	239+65.85	-34.99 LT	240909.86	462463.37	919.30
086	239+57.47	-41.33 LT	240898.42	462463.40	919.39
087	239+66.16	-40.57 LT	240907.13	462458.49	919.39
088	239+57.77	-45.03 LT	240896.60	462460.15	919.44
089	239+56.67	-47.91 LT	240893.90	462458.51	919.89
090	239+50.70	-44.99 LT	240889.91	462464.94	919.55
091	239+48.95	-49.56 LT	240885.56	462462.46	919.66
092	239+47.94	-52.35 LT	240882.93	462460.93	920.21
093	239+54.97	-52.49 LT	240889.63	462455.92	919.92
094	239+51.27	-62.90 LT	240879.95	462450.04	920.61
095	239+55.00	-64.87 LT	240882.54	462445.77	920.70
096	239+58.81	-54.47 LT	240892.27	462451.67	919.99
097	239+60.56	-49.92 LT	240896.55	462454.27	919.96
098	239+62.15	-45.92 LT	240900.32	462456.56	919.46
099	239+66.42	-45.57 LT	240904.69	462454.12	919.46

STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
100	239+75.29	-45.10 LT	240913.71	462449.07	920.14
101	239+75.17	-40.11 LT	240916.15	462453.43	920.03
102	239+46.21	-57.03 LT	240878.47	462458.42	920.22
103	239+71.65	-20.84 LT	240922.83	462471.92	919.24
104	239+84.61	-62.02 LT	240914.91	462428.86	921.49

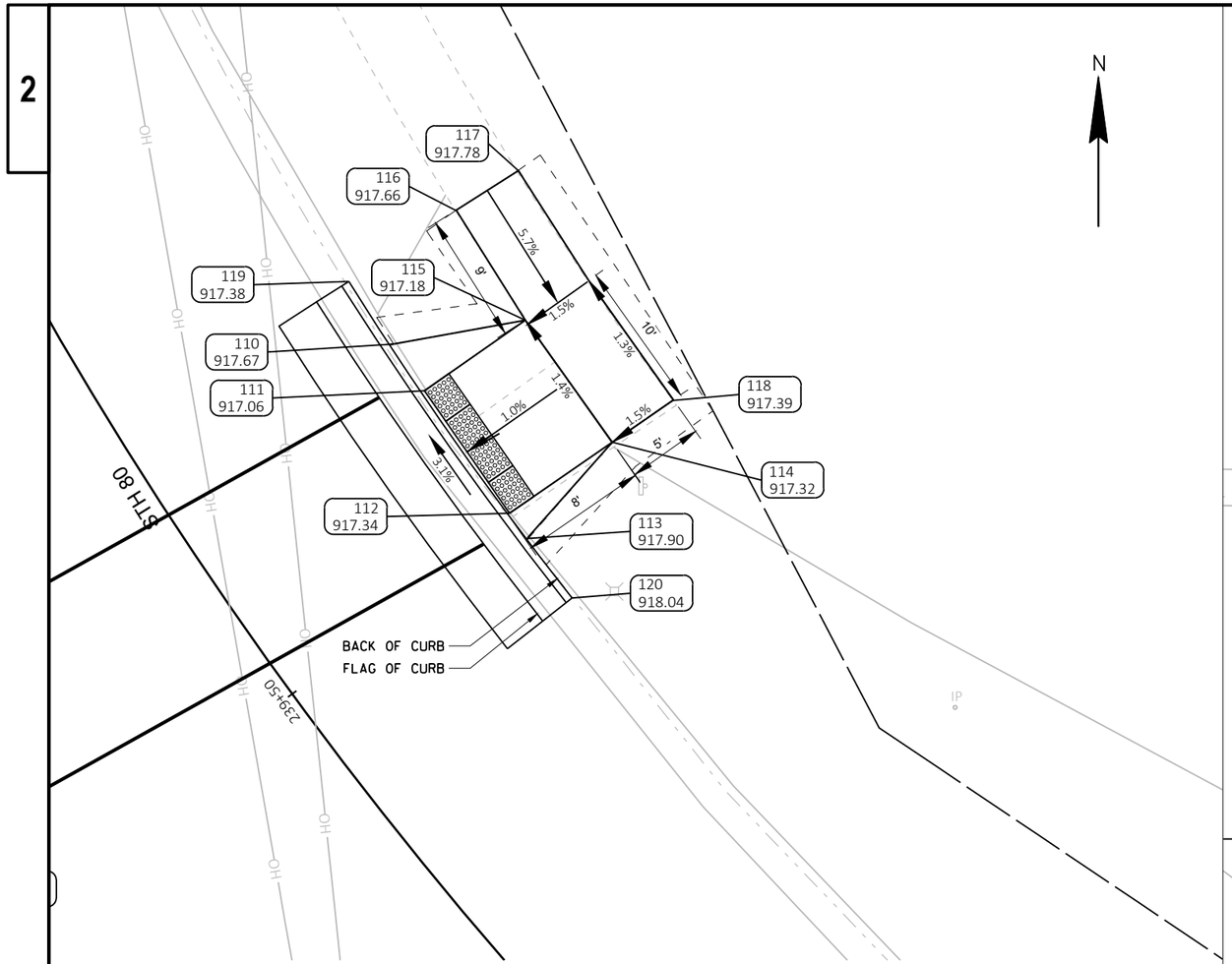
RADIAL TILE									
WARNING FIELD AREA (SF)	RADIUS AT BACK OF CURB (FT)	LONG CHORD				LANDING DISTANCE			
		POINT	-	POINT	LENGTH	POINT	-	POINT	LENGTH (FT)
26.66	44	081	-	082	12.66	081	-	085	10.6

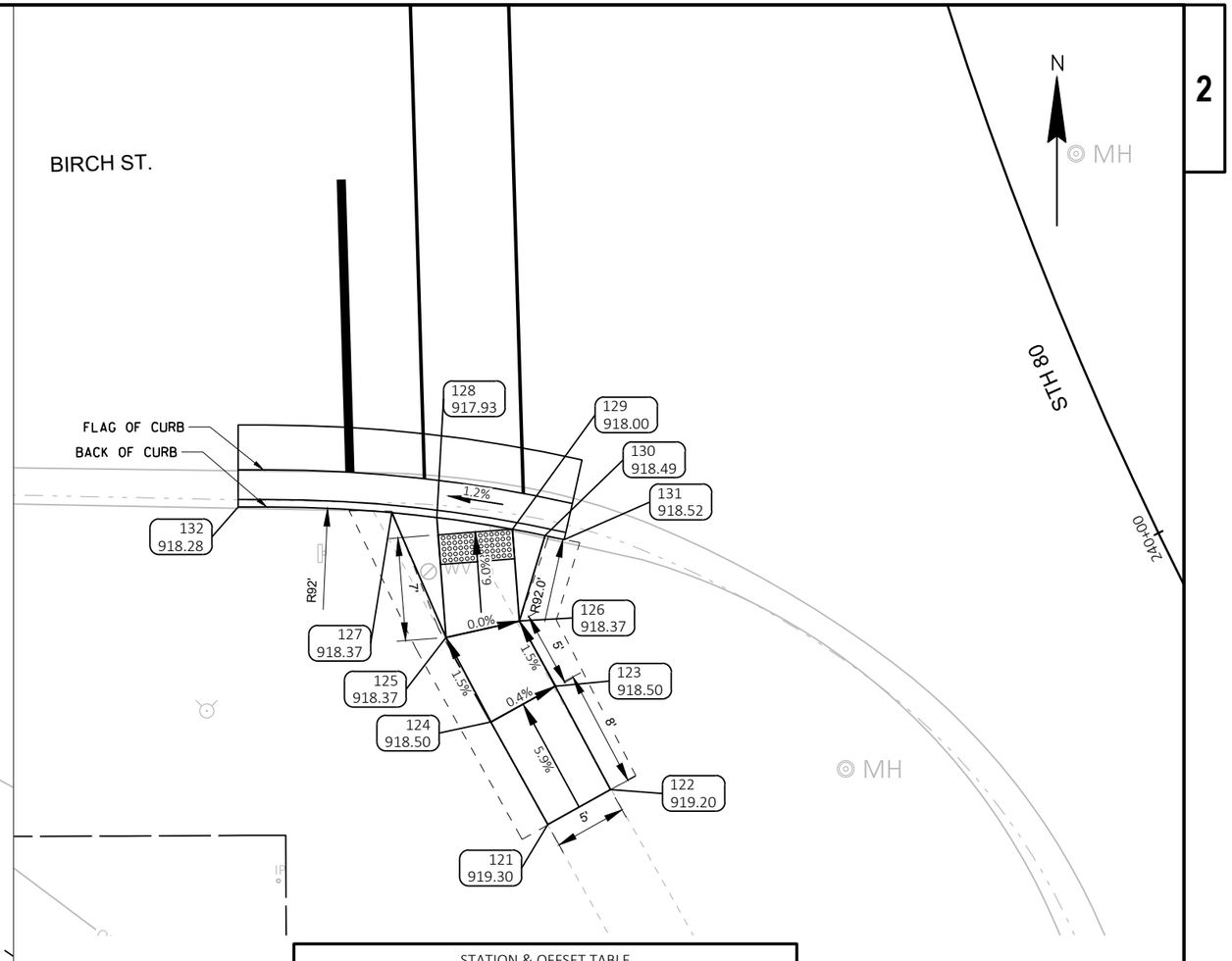
SCALE, FEET 0 5 10

PLUM ST. & STH 80
SW QUADRANT
CURB RAMP TYPE 3-PAVED FLARES

PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	CURB RAMP DETAILS	SHEET 16	E

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\021003_CD.DWG
LAYOUT NAME - 3
PLOT DATE : 7/5/2022 5:35 PM
PLOT BY : MARZOLF, ROBBI
PLOT NAME :
PLOT SCALE : ##########
WISDOT/CADDS SHEET 42





STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
110	239+66.04	18.91 RT	240939.27	462508.55	917.67
111	239+62.01	18.91 RT	240936.13	462510.60	917.06
112	239+51.27	18.91 RT	240927.92	462516.30	917.34
113	239+49.09	18.80 RT	240926.21	462517.42	917.90
114	239+51.07	27.28 RT	240932.68	462523.20	917.32
115	239+62.22	27.18 RT	240940.87	462517.38	917.18
116	239+71.89	27.21 RT	240948.23	462512.71	917.66
117	239+71.97	32.18 RT	240950.90	462516.90	917.78
118	239+50.94	32.23 RT	240935.49	462527.27	917.39
119	239+71.57	18.61 RT	240943.47	462505.54	917.38
120	239+43.70	18.88 RT	240922.24	462520.50	918.04

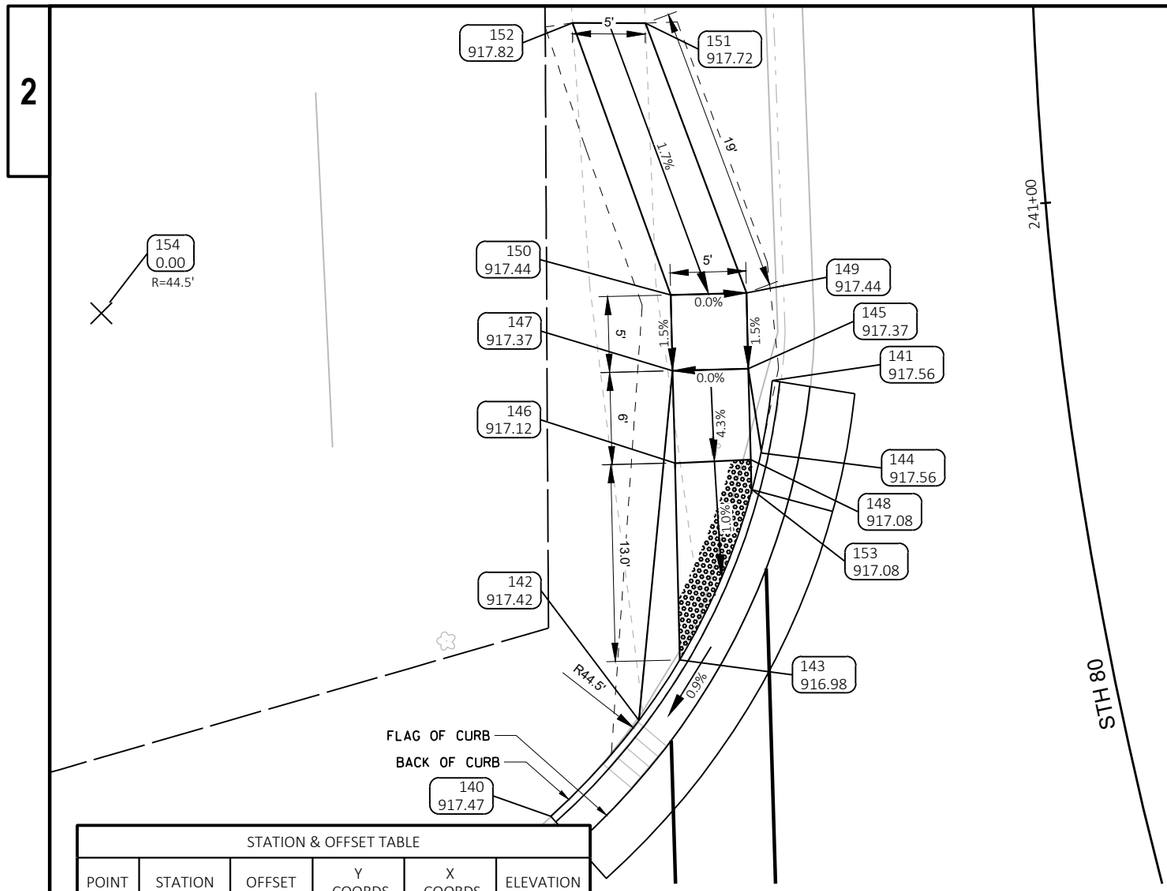
PLUM ST. & STH 80
NW QUADRANT
CURB RAMP TYPE 7B MID BLOCK CROSSING-PAVED FLARES

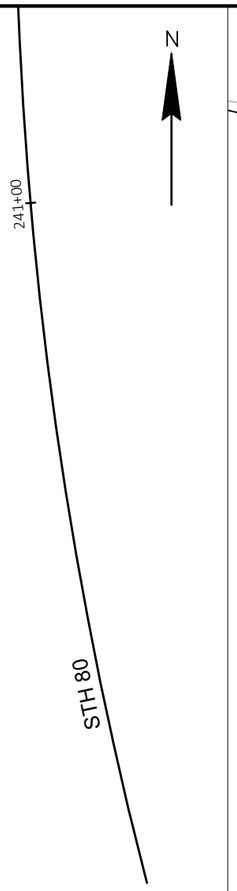
STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
121	240+00.18	-45.52 LT	240939.02	462434.96	919.30
122	240+00.43	-40.70 LT	240941.37	462439.18	919.20
123	240+07.28	-41.13 LT	240948.32	462435.47	918.50
124	240+06.92	-46.08 LT	240945.90	462431.13	918.50
125	240+12.42	-46.53 LT	240951.59	462428.12	918.37
126	240+11.61	-41.55 LT	240952.70	462433.07	918.37
127	240+20.25	-46.72 LT	240959.99	462424.47	918.37
128	240+19.02	-44.02 LT	240959.65	462427.50	917.93
129	240+16.74	-39.61 LT	240958.88	462432.58	918.00
130	240+15.66	-37.74 LT	240958.45	462434.78	918.49
131	240+14.99	-36.65 LT	240958.17	462436.07	918.52
132	240+23.71	-56.21 LT	240960.38	462414.13	918.28

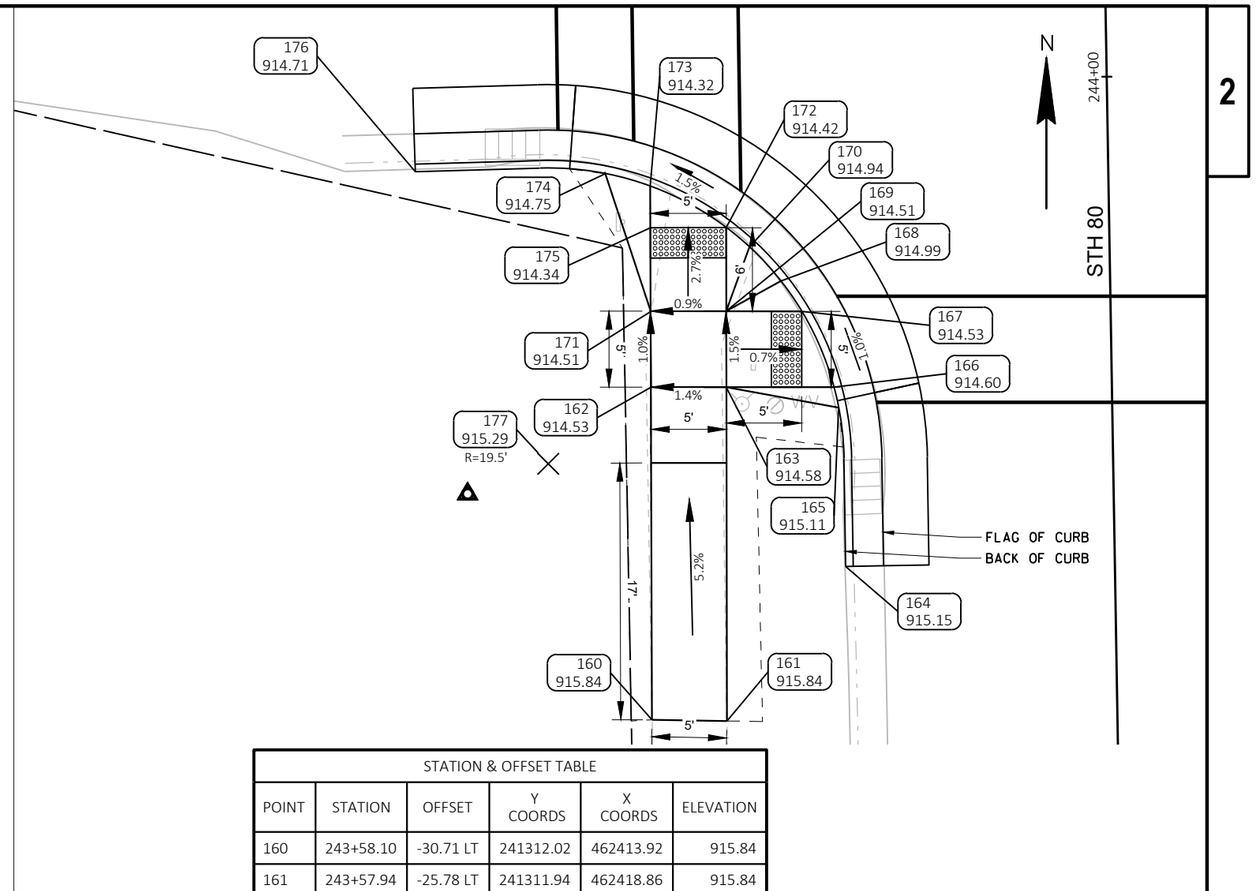
BIRCH ST. & STH 80
SW QUADRANT
CURB RAMP TYPE 4B1-PAVED FLARES

SCALE, FEET 0 5 10

PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	CURB RAMP DETAILS	SHEET 17	E







STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
140	240+67.02	-38.29 LT	241014.05	462417.02	917.47
141	240+90.55	-19.16 LT	241042.79	462431.64	917.56
142	240+71.51	-31.34 LT	241020.37	462422.83	917.42
143	240+74.60	-27.95 LT	241024.36	462425.54	916.98
144	240+86.20	-20.47 LT	241038.00	462430.94	917.56
145	240+91.44	-20.63 LT	241043.56	462430.06	917.37
146	240+86.32	-26.21 LT	241037.35	462425.24	917.12
147	240+91.86	-25.61 LT	241043.44	462425.07	917.37
148	240+85.89	-21.24 LT	241037.56	462430.22	917.08
149	240+96.08	-20.21 LT	241048.56	462429.94	917.44
150	240+96.42	-25.20 LT	241048.43	462424.94	917.44
151	241+12.90	-25.61 LT	241066.36	462423.27	917.72
152	241+13.09	-30.41 LT	241066.38	462418.46	917.82
153	240+84.07	-21.43 LT	241035.59	462430.32	917.08
154	240+98.50	-62.71 LT	241047.23	462387.36	0.00

BIRCH ST. & STH 80
NW QUADRANT
CURB RAMP TYPE 4B1-PAVED FLARES

RADIAL TILE									
WARNING FIELD AREA (SF)	RADIUS AT BACK OF CURB (FT)	LONG CHORD				LANDING DISTANCE			
		POINT	-	POINT	LENGTH	POINT	-	POINT	LENGTH (FT)
22.56	45	081	-	082	12.66	081	-	085	10.6

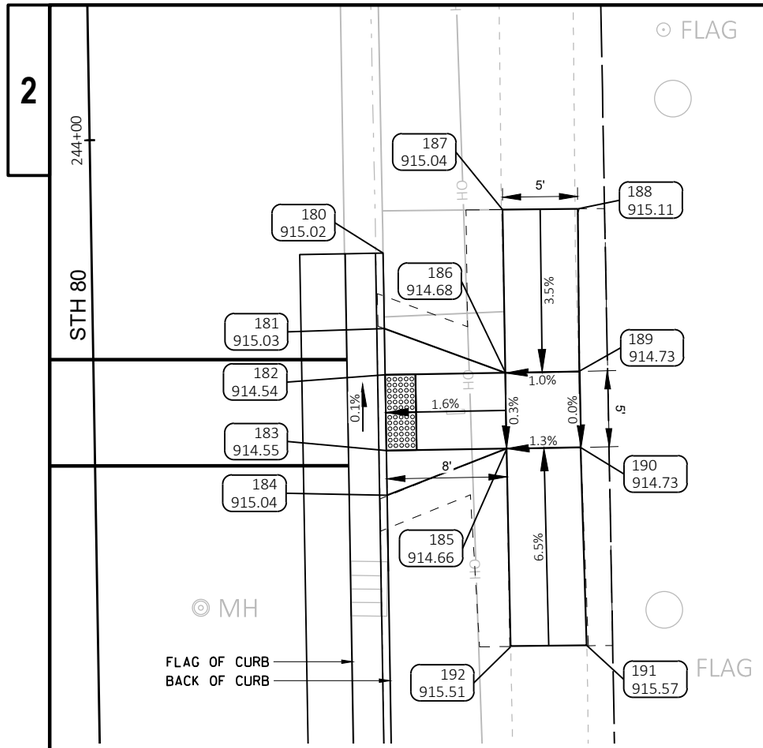
STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
160	243+58.10	-30.71 LT	241312.02	462413.92	915.84
161	243+57.94	-25.78 LT	241311.94	462418.86	915.84
162	243+80.03	-30.40 LT	241333.95	462413.86	914.53
163	243+79.95	-25.44 LT	241333.96	462418.82	914.58
164	243+68.00	-17.77 LT	241322.14	462426.70	915.15
165	243+78.48	-18.07 LT	241332.62	462426.22	915.11
166	243+79.83	-18.35 LT	241333.96	462425.91	914.60
167	243+84.86	-20.39 LT	241338.96	462423.79	914.53
168	243+86.89	-21.74 LT	241340.96	462422.41	914.99
169	243+84.96	-25.36 LT	241338.97	462418.82	914.51
170	243+89.17	-23.77 LT	241343.20	462420.34	914.94
171	243+85.06	-30.30 LT	241338.98	462413.88	914.51
172	243+90.48	-25.28 LT	241344.49	462418.81	914.42
173	243+93.29	-30.23 LT	241347.21	462413.80	914.32
174	243+94.17	-33.19 LT	241348.04	462410.84	914.75
175	243+90.56	-30.27 LT	241344.48	462413.81	914.34
176	243+94.50	-45.72 LT	241348.17	462398.30	914.71
177	243+75.10	-37.27 LT	241328.91	462407.08	915.29

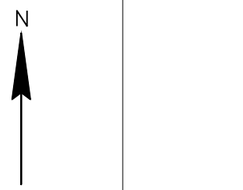
MAPLE ST. & STH 80
SW QUADRANT
CURB RAMP TYPE 2-PAVED FLARES

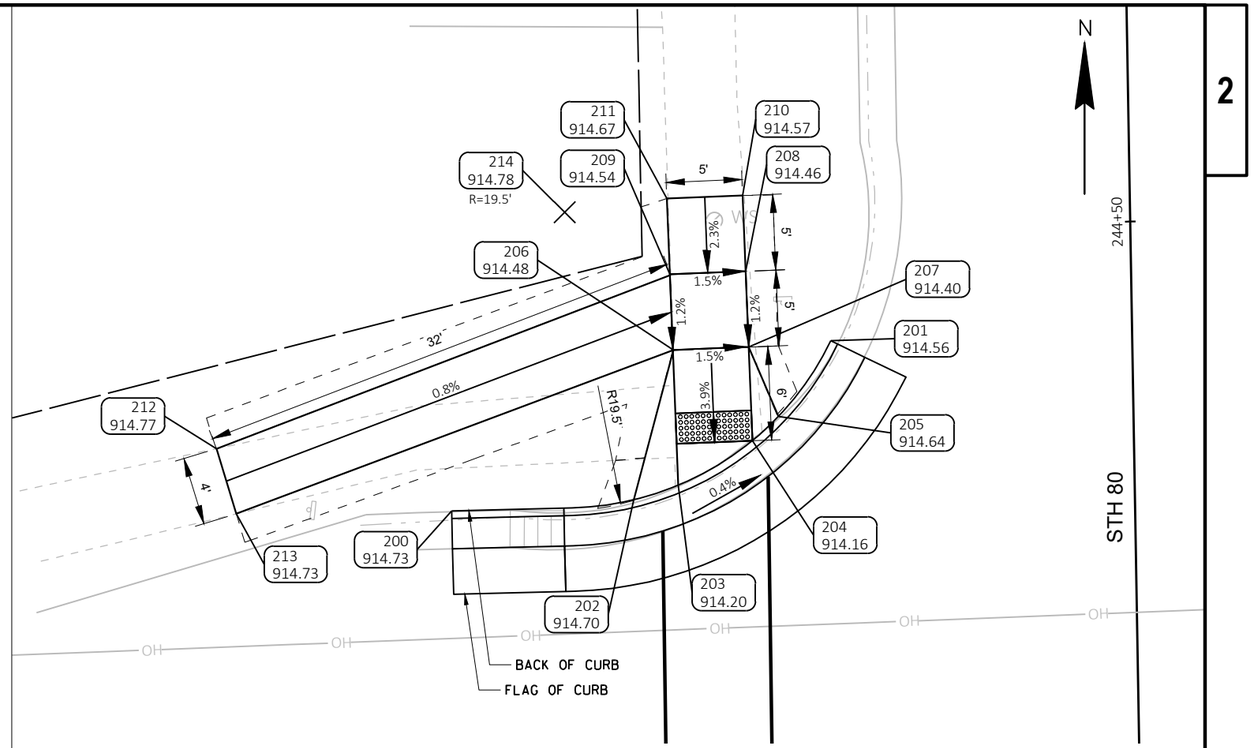
SCALE, FEET 0 5 10

PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	CURB RAMP DETAILS	SHEET 18	E

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\021003_CD.DWG
LAYOUT NAME - 5
PLOT DATE : 7/5/2022 5:35 PM
PLOT BY : MARZOLF, ROBBI
PLOT NAME :
PLOT SCALE : ##########
WISDOT/CADDS SHEET 42







STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
180	243+92.23	19.24 RT	241346.99	462463.29	915.02
181	243+87.26	19.24 RT	241342.02	462463.37	915.03
182	243+84.20	19.24 RT	241338.97	462463.43	914.54
183	243+79.20	19.24 RT	241333.97	462463.51	914.55
184	243+76.26	19.25 RT	241331.03	462463.57	915.04
185	243+79.20	27.19 RT	241334.10	462471.45	914.66
186	243+84.20	27.19 RT	241339.10	462471.37	914.68
187	243+94.97	27.18 RT	241349.87	462471.18	915.04
188	243+94.93	32.14 RT	241349.92	462476.14	915.11
189	243+84.20	32.03 RT	241339.18	462476.22	914.73
190	243+79.20	32.03 RT	241334.18	462476.30	914.73
191	243+66.13	32.24 RT	241321.12	462476.73	915.57
192	243+66.12	27.22 RT	241321.02	462471.71	915.51

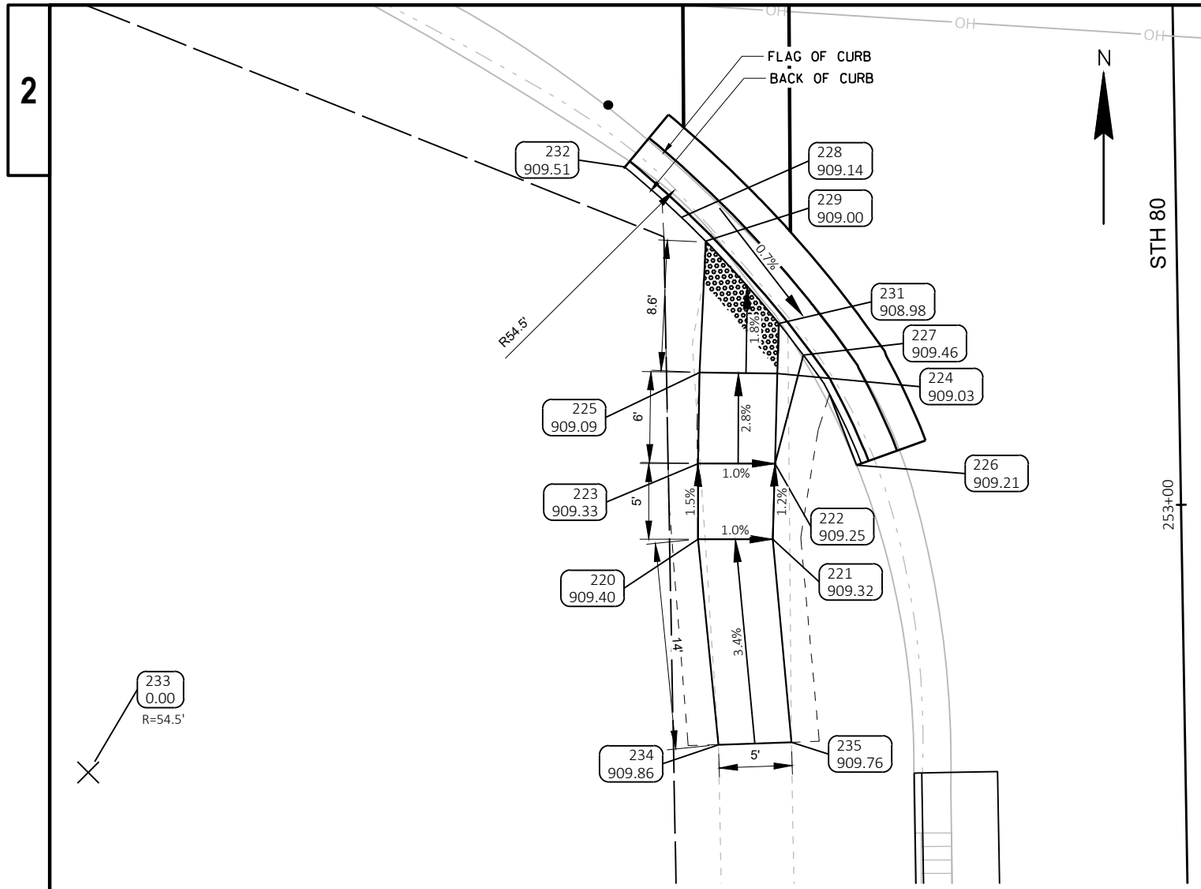
MAPLE ST. & STH 80
SE QUADRANT
CURB RAMP TYPE 7B MID BLOCK CROSSING-PAVED FLARES

STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
200	244+31.68	-45.09 LT	241385.35	462398.30	914.73
201	244+42.47	-19.88 LT	241396.57	462423.32	914.56
202	244+32.21	-33.06 LT	241386.08	462410.32	914.70
203	244+33.12	-30.11 LT	241387.04	462413.26	914.20
204	244+35.98	-25.17 LT	241389.98	462418.14	914.16
205	244+37.55	-23.42 LT	241391.58	462419.87	914.64
206	244+42.03	-30.29 LT	241395.95	462412.92	914.48
207	244+42.15	-25.31 LT	241396.15	462417.90	914.40
208	244+47.15	-25.43 LT	241401.15	462417.69	914.46
209	244+47.04	-30.43 LT	241400.95	462412.70	914.54
210	244+52.15	-25.55 LT	241406.15	462417.49	914.57
211	244+52.03	-30.55 LT	241405.95	462412.50	914.67
212	244+36.04	-60.54 LT	241389.45	462382.78	914.77
213	244+31.73	-59.30 LT	241385.16	462384.09	914.73
214	244+51.24	-37.30 LT	241405.04	462405.76	914.78

MAPLE ST. & STH 80
NW QUADRANT
CURB RAMP TYPE 4B1-PAVED FLARES

SCALE, FEET 0 5 10

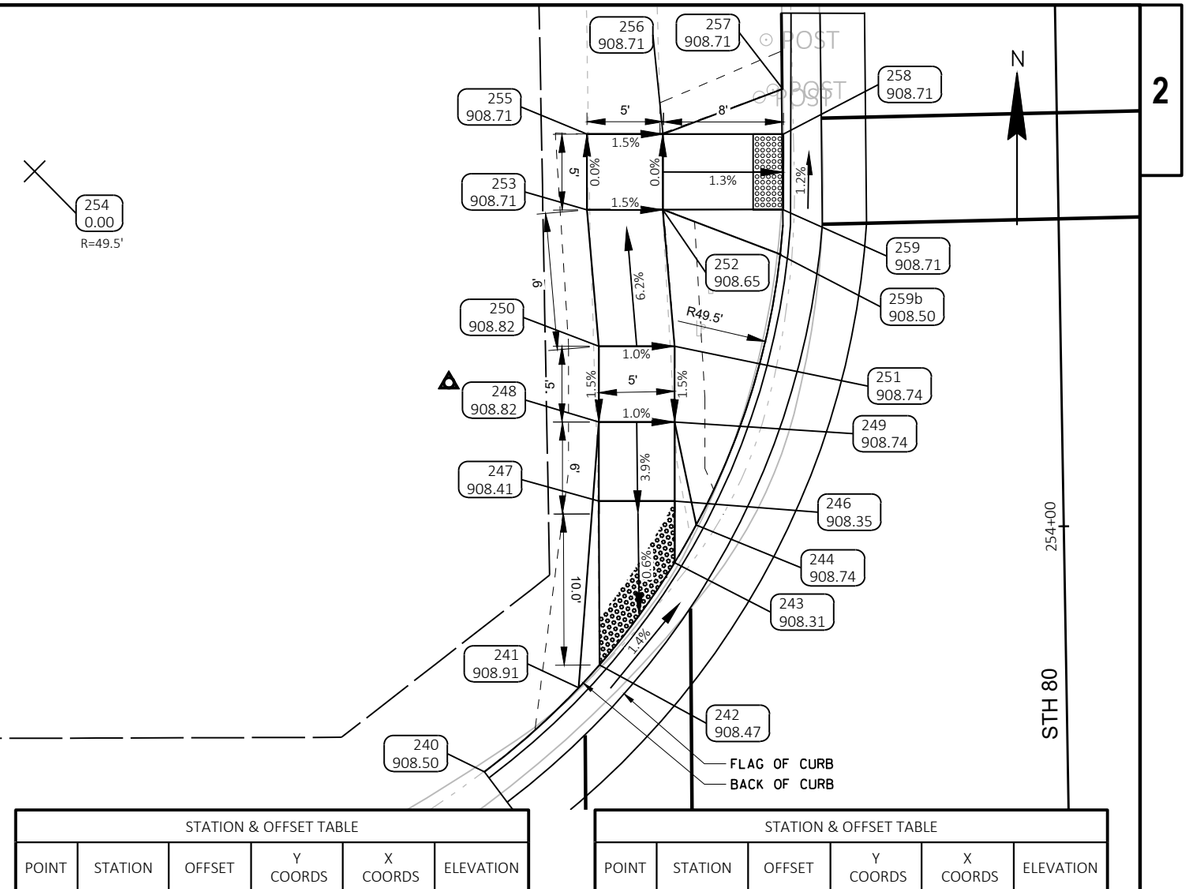
PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	CURB RAMP DETAILS	SHEET 19	E



STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
220	252+98.27	-31.91 LT	242252.04	462396.80	909.40
221	252+98.18	-26.98 LT	242252.04	462401.73	909.32
222	253+03.17	-26.75 LT	242257.02	462401.88	909.25
223	253+03.25	-31.82 LT	242257.02	462396.80	909.33
224	253+09.11	-26.48 LT	242262.96	462402.04	909.03
225	253+09.27	-31.63 LT	242263.04	462396.90	909.09
226	253+05.76	-22.63 LT	242256.91	462407.27	909.21
227	253+10.30	-24.75 LT	242264.19	462403.76	909.46
228	253+19.51	-32.64 LT	242273.27	462395.71	909.14
229	253+17.92	-31.08 LT	242271.70	462397.30	909.00
231	253+12.41	-26.33 LT	242266.27	462402.14	908.98
232	253+22.90	-36.32 LT	242276.59	462391.97	909.51
233	252+83.56	-72.40 LT	242236.65	462356.56	0.00
234	252+84.69	-30.79 LT	242238.47	462398.15	909.86
235	252+84.77	-25.97 LT	242238.64	462402.96	909.76

RADIAL TILE									
WARNING FIELD AREA (SF)	RADIUS AT BACK OF CURB (FT)	LONG CHORD				LANDING DISTANCE			
		POINT	-	POINT	LENGTH	POINT	-	POINT	LENGTH (FT)
13.13	55	229	-	231	7.27	225	-	229	8.6

KING. & STH 80
SW QUADRANT
CURB RAMP TYPE 4B1-PAVED FLARES



STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
240	253+84.46	-38.49 LT	242338.10	462388.76	908.50
241	253+89.89	-32.18 LT	242343.64	462394.98	908.91
242	253+91.37	-30.78 LT	242345.14	462396.35	908.47
243	253+98.03	-25.71 LT	242351.89	462401.31	908.31
244	254+00.50	-24.24 LT	242354.38	462402.74	908.74
246	254+01.32	-25.66 LT	242355.96	462401.31	908.35
247	254+01.34	-30.66 LT	242355.96	462396.31	908.41
248	254+07.40	-30.55 LT	242361.18	462396.31	908.82
249	254+07.32	-25.56 LT	242361.18	462401.30	908.74
250	254+12.40	-30.47 LT	242366.18	462396.31	908.82
251	254+12.32	-25.47 LT	242366.18	462401.31	908.74
252	254+21.34	-26.09 LT	242375.19	462400.53	908.65
253	254+21.41	-31.09 LT	242375.17	462395.53	908.71
254	254+24.57	-67.49 LT	242377.72	462359.08	0.00
255	254+26.41	-31.01 LT	242380.17	462395.53	908.71

STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
256	254+26.33	-26.01 LT	242380.17	462400.53	908.71
257	254+29.18	-18.05 LT	242383.16	462408.45	908.71
258	254+26.19	-18.06 LT	242380.17	462408.48	908.71
259B	254+18.34	-18.58 LT	242372.31	462408.10	908.50
259	254+21.19	-18.14 LT	242375.17	462408.48	908.71

RADIAL TILE									
WARNING FIELD AREA (SF)	RADIUS AT BACK OF CURB (FT)	LONG CHORD				LANDING DISTANCE			
		POINT	-	POINT	LENGTH	POINT	-	POINT	LENGTH (FT)
17.51	50	242	-	243	8.38	242	-	247	10.0

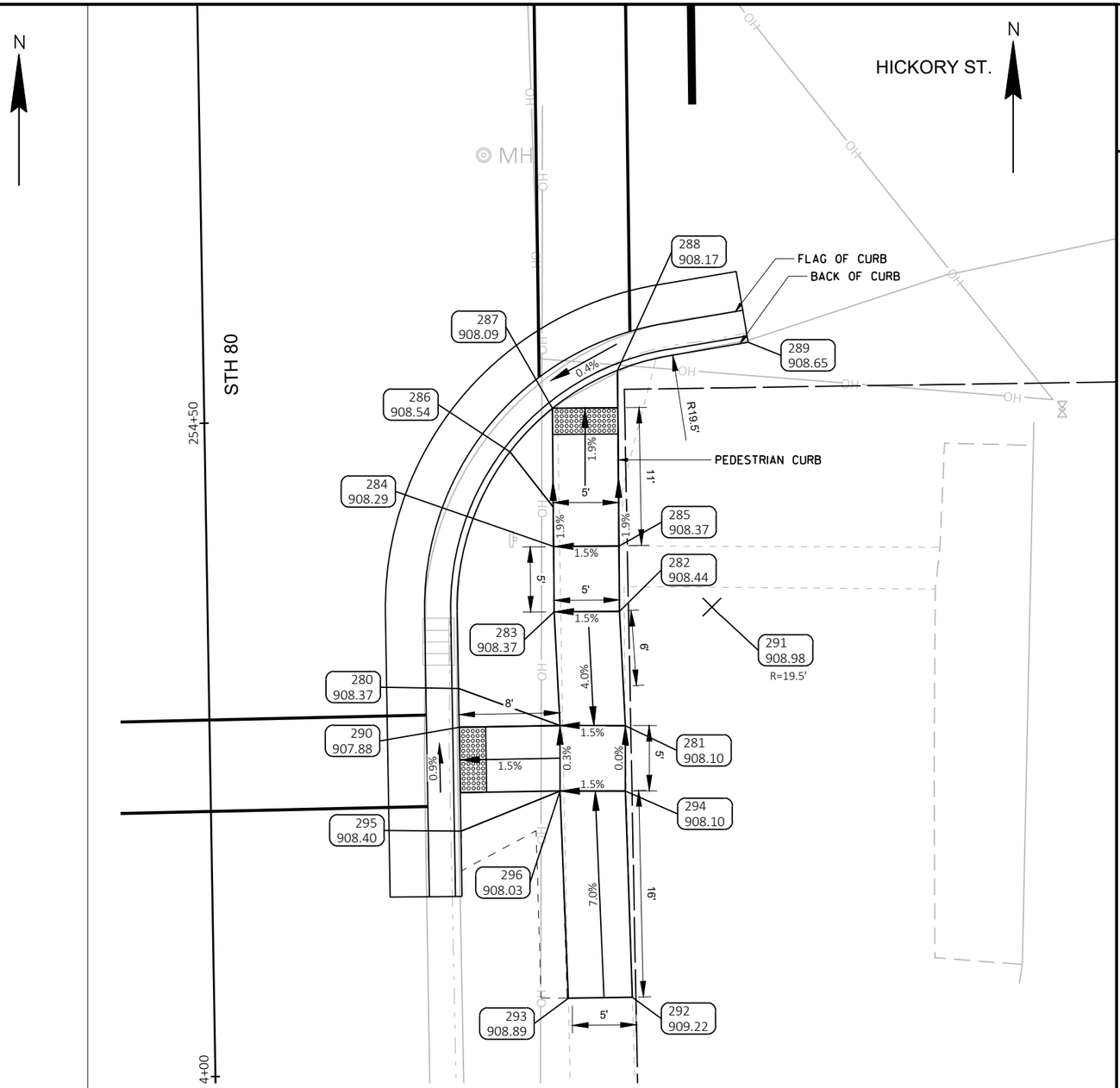
SCALE, FEET 0 5 10

KING. & STH 80
NW QUADRANT
CURB RAMP TYPE 4B1-PAVED FLARES

PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	CURB RAMP DETAILS	SHEET 20	E

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\021003_CD.DWG
LAYOUT NAME - 7
PLOT DATE : 7/5/2022 5:36 PM
PLOT BY : MARZOLF, ROBBI
PLOT NAME :
PLOT SCALE : ##########
WISDOT/CADDS SHEET 42

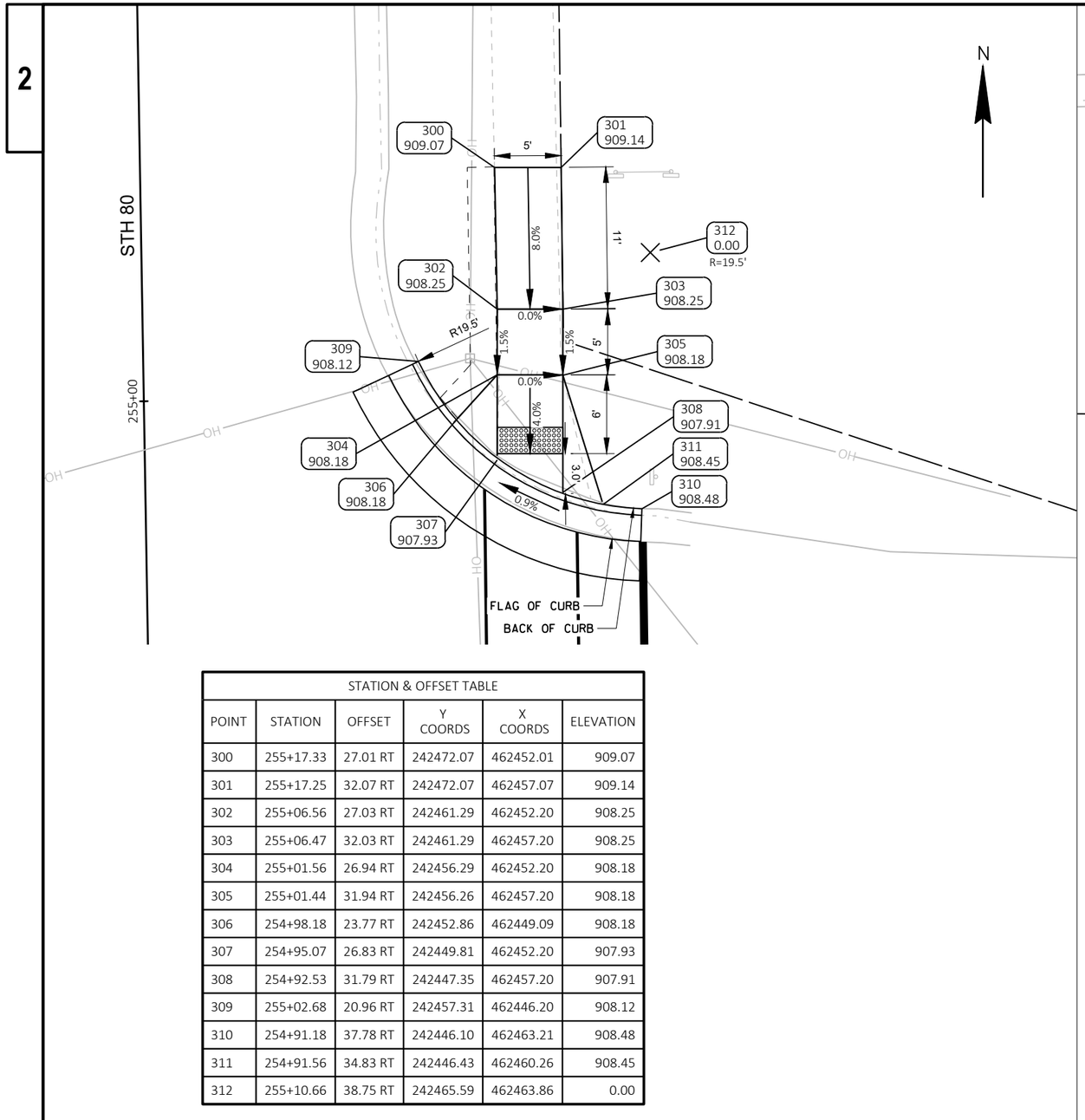
STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
280	254+29.38	19.13 RT	242383.99	462445.62	908.37
281	254+26.32	31.83 RT	242381.14	462458.36	908.10
282	254+35.07	31.53 RT	242389.89	462457.91	908.44
283	254+35.12	26.53 RT	242389.85	462452.91	908.37
284	254+40.12	26.58 RT	242394.85	462452.88	908.29
285	254+40.07	31.58 RT	242394.89	462457.88	908.37
286	254+47.39	23.36 RT	242402.08	462449.54	908.54
287	254+50.69	26.67 RT	242405.43	462452.79	908.09
288	254+53.51	31.69 RT	242408.33	462457.77	908.17
289	254+55.48	41.73 RT	242410.47	462467.77	908.65
290	254+26.40	19.13 RT	242381.01	462445.67	907.88
291	254+35.28	38.61 RT	242390.22	462464.99	908.98
292	254+05.52	32.02 RT	242360.36	462458.90	909.22
293	254+05.54	27.12 RT	242360.30	462454.00	908.89
294	254+21.32	31.75 RT	242376.14	462458.36	908.10
295	254+18.45	19.14 RT	242373.07	462445.80	908.40
296	254+21.40	26.75 RT	242376.14	462453.36	908.03



SCALE, FEET 0 5 10

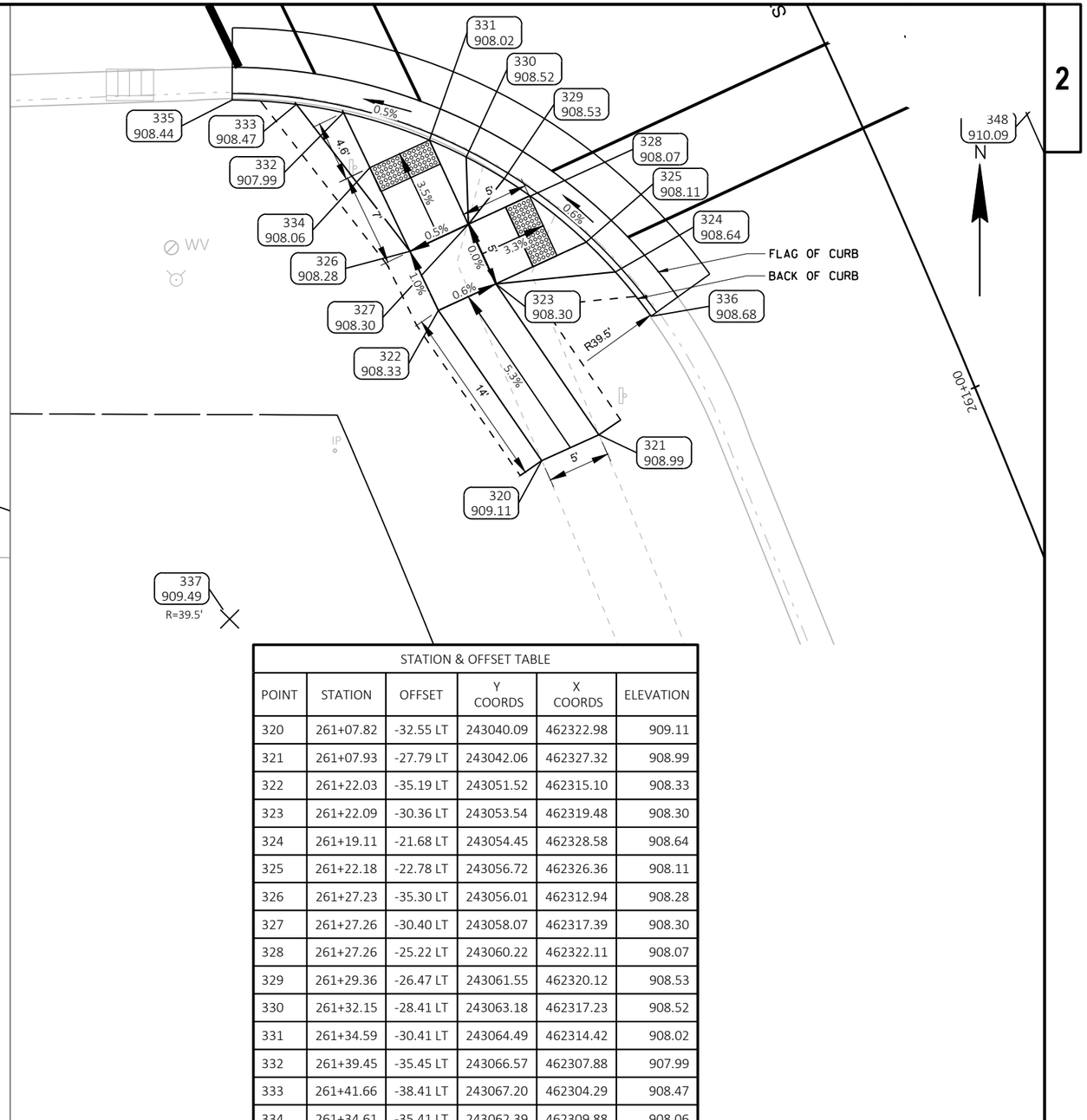
HICKORY ST. & STH 80
SE QUADRANT
CURB RAMP TYPE 4B1-PAVED FLARES

PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	CURB RAMP DETAILS	SHEET 21	E



STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
300	255+17.33	27.01 RT	242472.07	462452.01	909.07
301	255+17.25	32.07 RT	242472.07	462457.07	909.14
302	255+06.56	27.03 RT	242461.29	462452.20	908.25
303	255+06.47	32.03 RT	242461.29	462457.20	908.25
304	255+01.56	26.94 RT	242456.29	462452.20	908.18
305	255+01.44	31.94 RT	242456.26	462457.20	908.18
306	254+98.18	23.77 RT	242452.86	462449.09	908.18
307	254+95.07	26.83 RT	242449.81	462452.20	907.93
308	254+92.53	31.79 RT	242447.35	462457.20	907.91
309	255+02.68	20.96 RT	242457.31	462446.20	908.12
310	254+91.18	37.78 RT	242446.10	462463.21	908.48
311	254+91.56	34.83 RT	242446.43	462460.26	908.45
312	255+10.66	38.75 RT	242465.59	462463.86	0.00

HICKORY ST. & STH 80
NE QUADRANT
CURB RAMP TYPE 4B1-PAVED FLARES

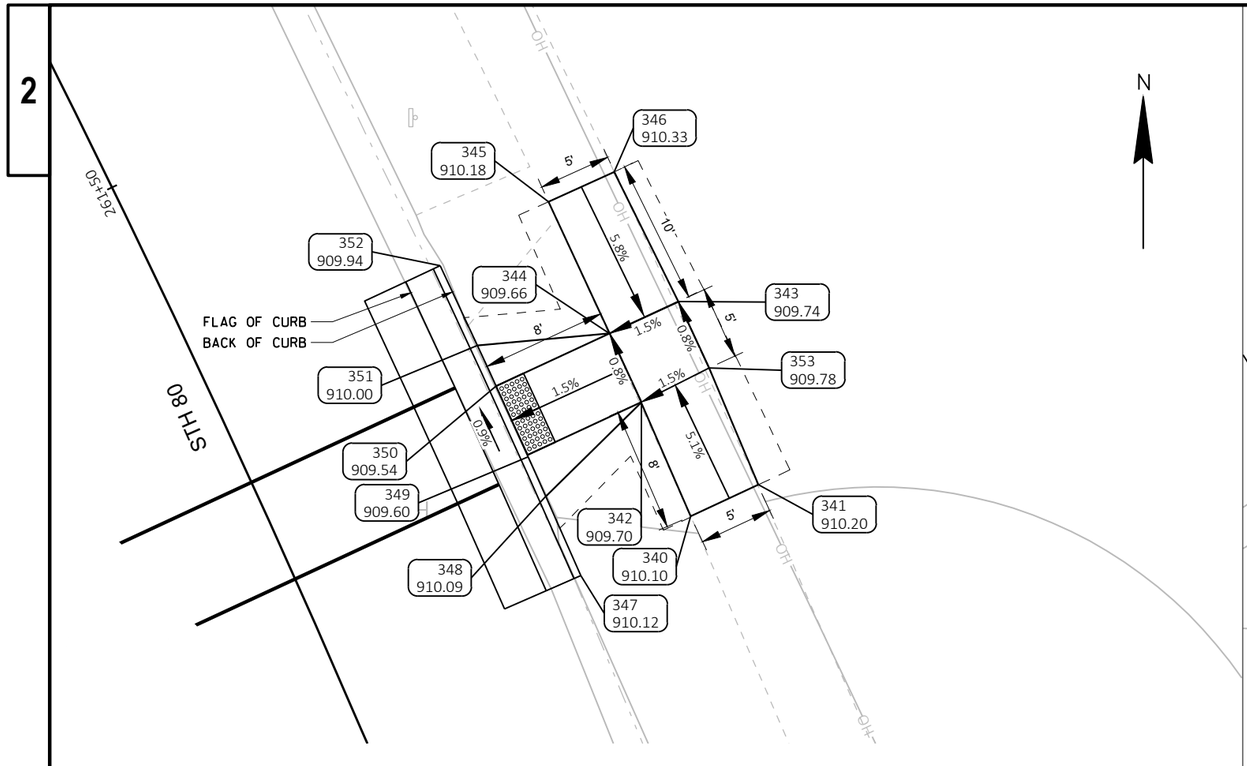


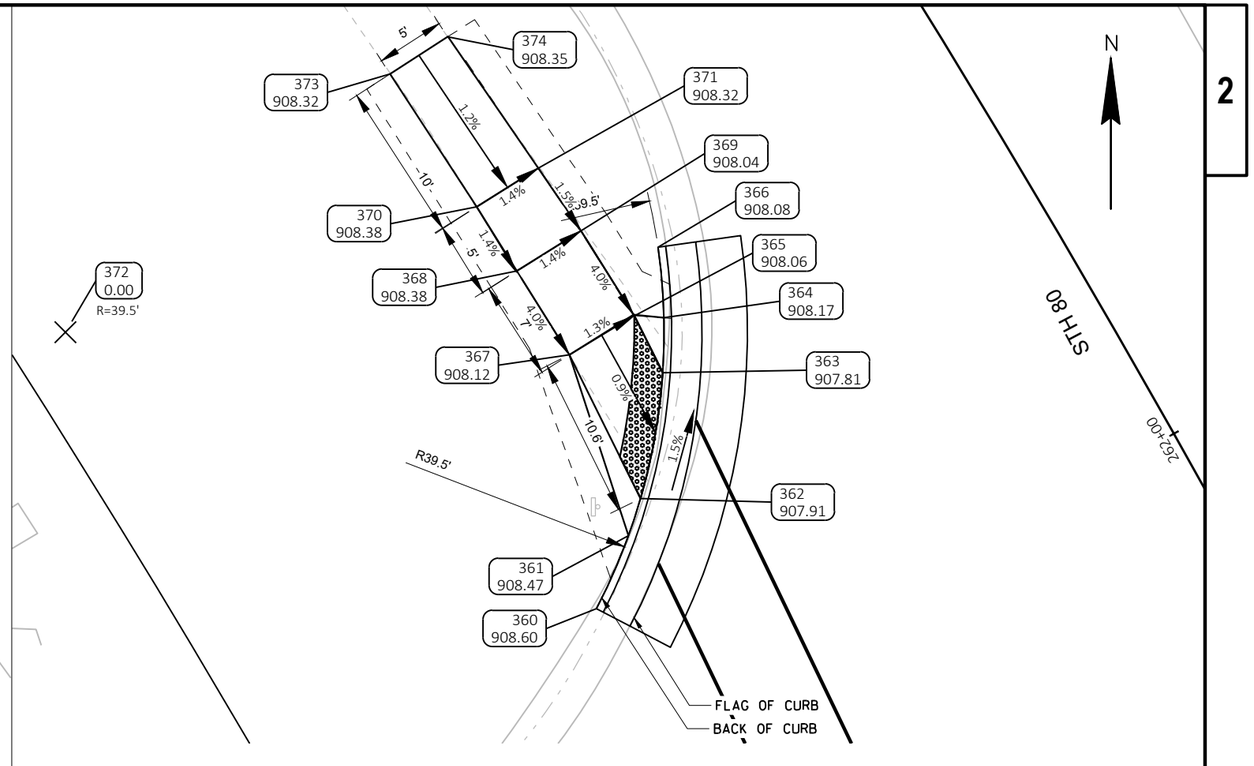
STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
320	261+07.82	-32.55 LT	243040.09	462322.98	909.11
321	261+07.93	-27.79 LT	243042.06	462327.32	908.99
322	261+22.03	-35.19 LT	243051.52	462315.10	908.33
323	261+22.09	-30.36 LT	243053.54	462319.48	908.30
324	261+19.11	-21.68 LT	243054.45	462328.58	908.64
325	261+22.18	-22.78 LT	243056.72	462326.36	908.11
326	261+27.23	-35.30 LT	243056.01	462312.94	908.28
327	261+27.26	-30.40 LT	243058.07	462317.39	908.30
328	261+27.26	-25.22 LT	243060.22	462322.11	908.07
329	261+29.36	-26.47 LT	243061.55	462320.12	908.53
330	261+32.15	-28.41 LT	243063.18	462317.23	908.52
331	261+34.59	-30.41 LT	243064.49	462314.42	908.02
332	261+39.45	-35.45 LT	243066.57	462307.88	907.99
333	261+41.66	-38.41 LT	243067.20	462304.29	908.47
334	261+34.61	-35.41 LT	243062.39	462309.88	908.06
335	261+44.19	-42.69 LT	243067.53	462299.40	908.44
336	261+14.83	-20.56 LT	243051.08	462331.29	908.68
337	261+05.96	-59.14 LT	243028.03	462299.21	909.49

W MIDDLE ST. & STH 80
SW QUADRANT
CURB RAMP TYPE 2-PAVED FLARES

SCALE, FEET 0 5 10

PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	CURB RAMP DETAILS	SHEET 22	E





STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
340	261+14.80	25.54 RT	243069.54	462373.54	910.10
341	261+14.92	30.47 RT	243071.63	462378.00	910.20
342	261+22.76	25.62 RT	243077.07	462370.28	909.70
343	261+27.63	30.58 RT	243083.70	462372.73	909.74
344	261+27.60	25.58 RT	243081.59	462368.19	909.66
345	261+36.91	25.56 RT	243090.29	462364.14	910.18
346	261+36.85	30.33 RT	243092.25	462368.49	910.33
347	261+14.14	17.34 RT	243065.63	462366.29	910.12
348	261+19.76	17.39 RT	243070.90	462364.02	910.09
349	261+22.54	17.33 RT	243073.46	462362.81	909.60
350	261+27.56	17.32 RT	243078.13	462360.69	909.54
351	261+30.45	17.31 RT	243080.81	462359.46	910.00
352	261+36.14	17.29 RT	243086.06	462356.99	909.94
353	261+22.98	30.61 RT	243079.32	462374.74	909.78

W MIDDLE ST. & STH 80
SE QUADRANT
CURB RAMP TYPE 7B MID BLOCK CROSSING-PAVED FLARES

STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
360	262+09.10	-38.82 LT	243123.93	462273.97	908.60
361	262+12.34	-34.56 LT	243128.75	462276.09	908.47
362	262+14.12	-32.64 LT	243131.20	462276.88	907.91
363	262+20.75	-27.16 LT	243139.49	462278.35	907.81
364	262+23.93	-25.22 LT	243143.13	462278.43	908.17
365	262+25.07	-26.83 LT	243143.24	462276.47	908.06
366	262+28.22	-23.14 LT	243147.77	462278.04	908.08
367	262+25.09	-31.84 LT	243140.67	462272.18	908.12
368	262+31.87	-31.90 LT	243146.21	462268.72	908.38
369	262+31.91	-26.90 LT	243148.86	462272.96	908.04
370	262+37.07	-31.92 LT	243150.45	462266.07	908.38
371	262+37.07	-27.14 LT	243152.98	462270.13	908.32
372	262+44.98	-59.29 LT	243142.18	462238.94	0.00
373	262+47.94	-32.07 LT	243159.20	462260.35	908.32
374	262+47.94	-27.55 LT	243161.65	462264.15	908.35

RADIAL TILE									
WARNING FIELD AREA (SF)	RADIUS AT BACK OF CURB (FT)	LONG CHORD				LANDING DISTANCE			
		POINT	-	POINT	LENGTH	POINT	-	POINT	LENGTH (FT)
17.91	40	362	-	363	8.37	362	-	367	10.6

SCALE, FEET 0 5 10

W MIDDLE ST. & STH 80
NW QUADRANT
CURB RAMP TYPE 4B1-PAVED FLARES

PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	CURB RAMP DETAILS	SHEET 23	E

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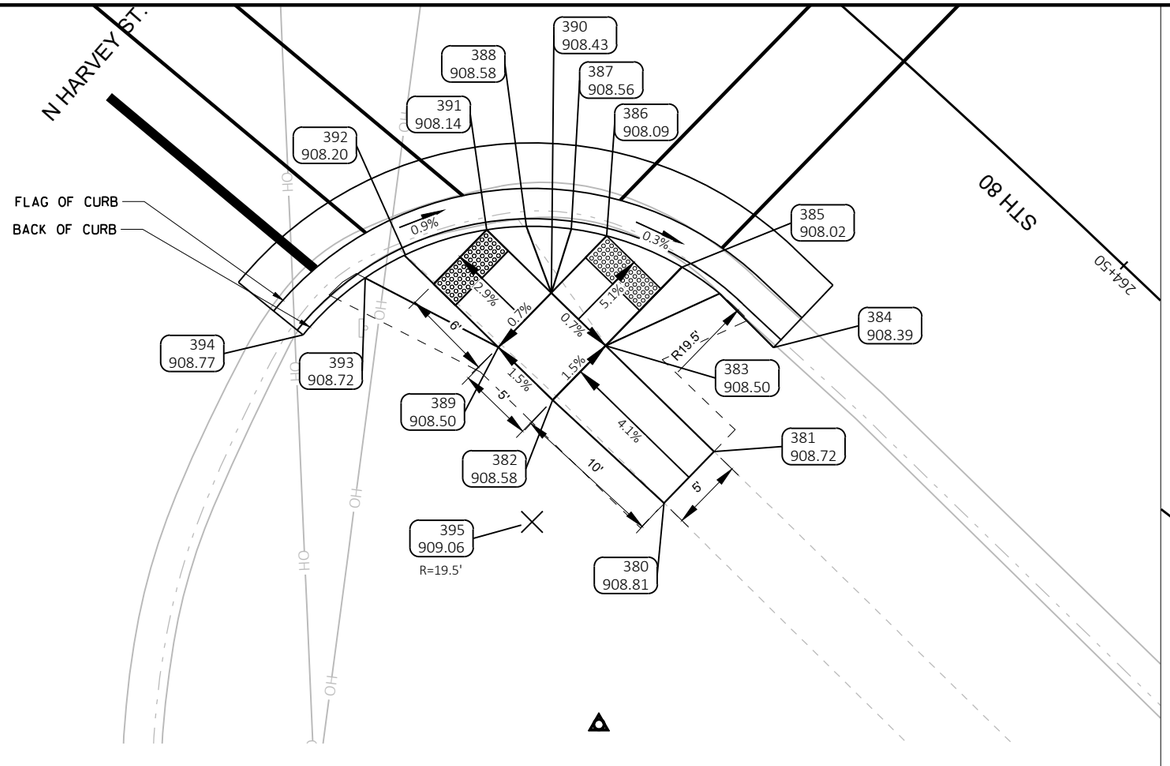
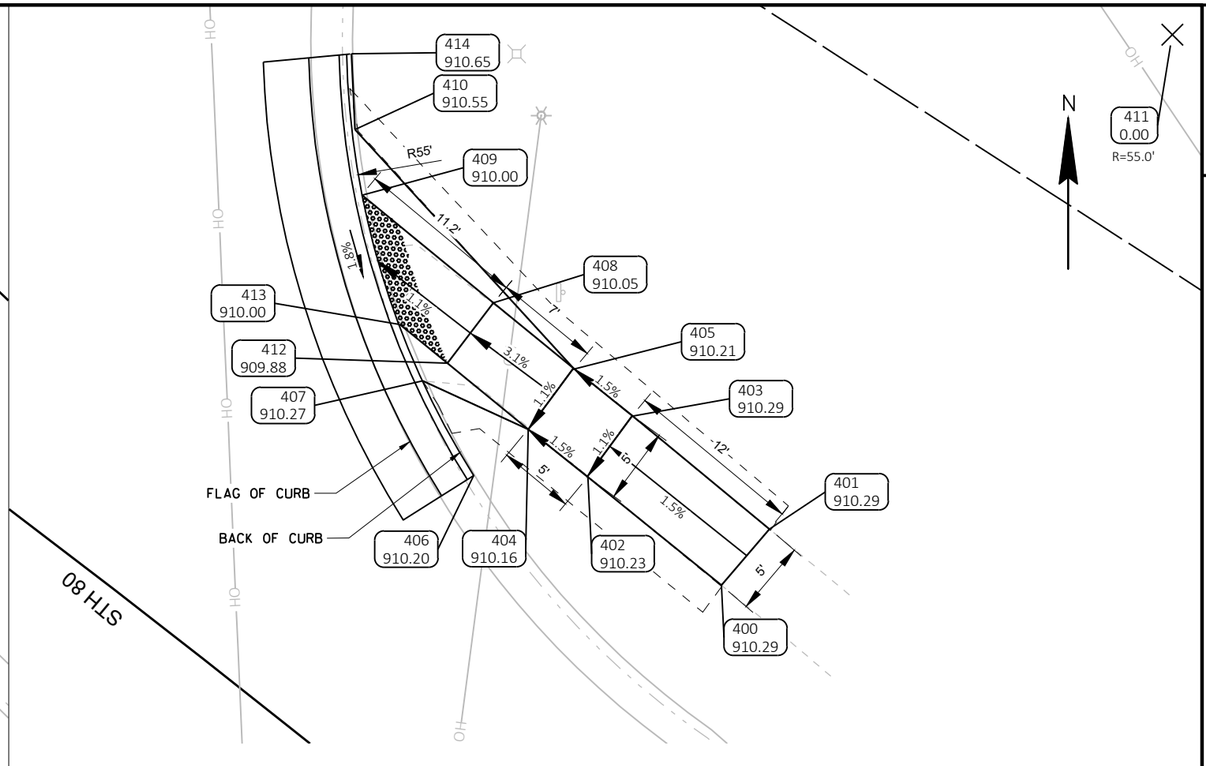
WISDOT/CADDS SHEET 42





STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
380	264+61.67	-32.18 LT	243316.17	462128.50	908.81
381	264+61.54	-27.45 LT	243319.57	462131.80	908.72
382	264+72.07	-32.12 LT	243322.97	462121.15	908.58
383	264+71.87	-27.12 LT	243326.55	462124.64	908.50
384	264+63.35	-19.74 LT	243326.43	462135.73	908.39
385	264+71.58	-19.90 LT	243331.73	462129.68	908.02
386	264+76.79	-21.67 LT	243333.79	462124.71	908.09
387	264+78.90	-22.88 LT	243334.25	462122.37	908.56
388	264+81.30	-24.71 LT	243334.42	462119.42	908.58
389	264+77.27	-31.90 LT	243326.45	462117.57	908.50
390	264+77.03	-26.91 LT	243330.04	462121.06	908.43
391	264+83.18	-26.62 LT	243334.20	462116.79	908.14
392	264+86.24	-31.38 LT	243332.55	462111.43	908.20
393	264+87.28	-34.28 LT	243331.02	462108.77	908.72
394	264+87.94	-39.81 LT	243327.26	462104.68	908.77
395	264+67.50	-38.99 LT	243314.93	462119.78	909.06

N HARVEY ST. & STH 80
SE QUADRANT
CURB RAMP TYPE 2-PAVED FLARES

STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
400	265+05.45	25.26 RT	243388.10	462133.55	910.29
401	265+05.38	30.14 RT	243391.80	462136.72	910.29
402	265+16.50	25.24 RT	243395.30	462124.73	910.23
403	265+16.71	30.14 RT	243399.24	462127.65	910.29
404	265+21.35	25.19 RT	243398.39	462120.81	910.16
405	265+21.53	30.08 RT	243402.33	462123.72	910.21
406	265+22.23	20.55 RT	243395.33	462117.21	910.20
407	265+28.58	23.36 RT	243401.57	462113.84	910.27
408	265+28.10	30.29 RT	243406.72	462118.51	910.05
409	265+38.92	30.59 RT	243413.83	462109.84	910.00

STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
410	265+41.81	33.75 RT	243418.16	462109.38	910.55
411	265+05.75	72.21 RT	243424.41	462163.30	0.00
412	265+28.03	25.29 RT	243402.75	462115.48	909.88
413	265+31.95	25.37 RT	243405.30	462112.35	910.00
414	265+44.88	37.59 RT	243423.15	462109.14	910.65

RADIAL TILE									
WARNING FIELD AREA (SF)	RADIUS AT BACK OF CURB (FT)	LONG CHORD				LANDING DISTANCE			
		POINT	-	POINT	LENGTH	POINT	-	POINT	LENGTH (FT)
18.3	40	409	-	413	8.72	408	-	409	11.2

N HARVEY ST. & STH 80
NE QUADRANT
CURB RAMP TYPE 4B1-PAVED FLARES

SCALE, FEET 0 5 10

PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	CURB RAMP DETAILS	SHEET 24	E

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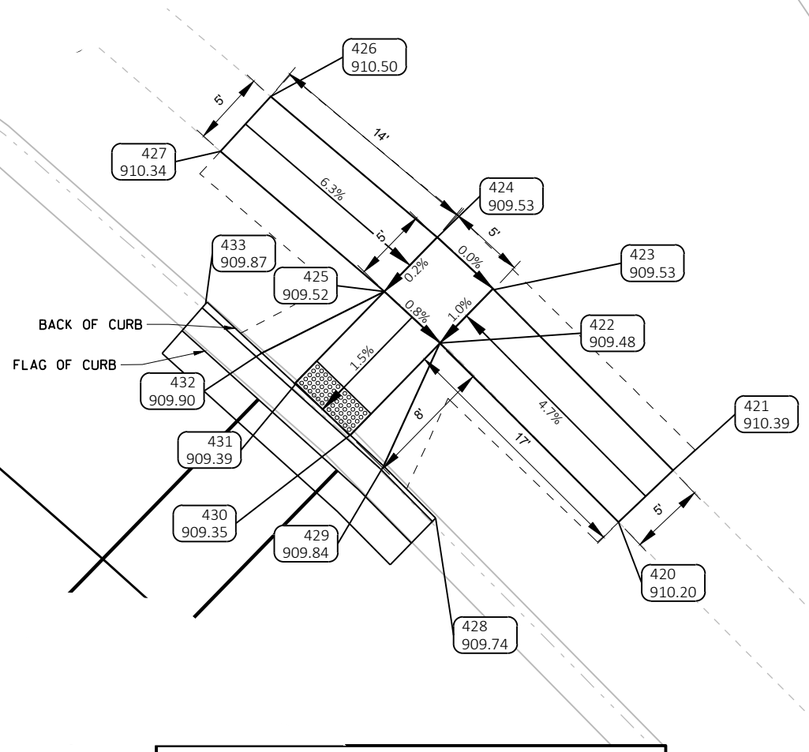
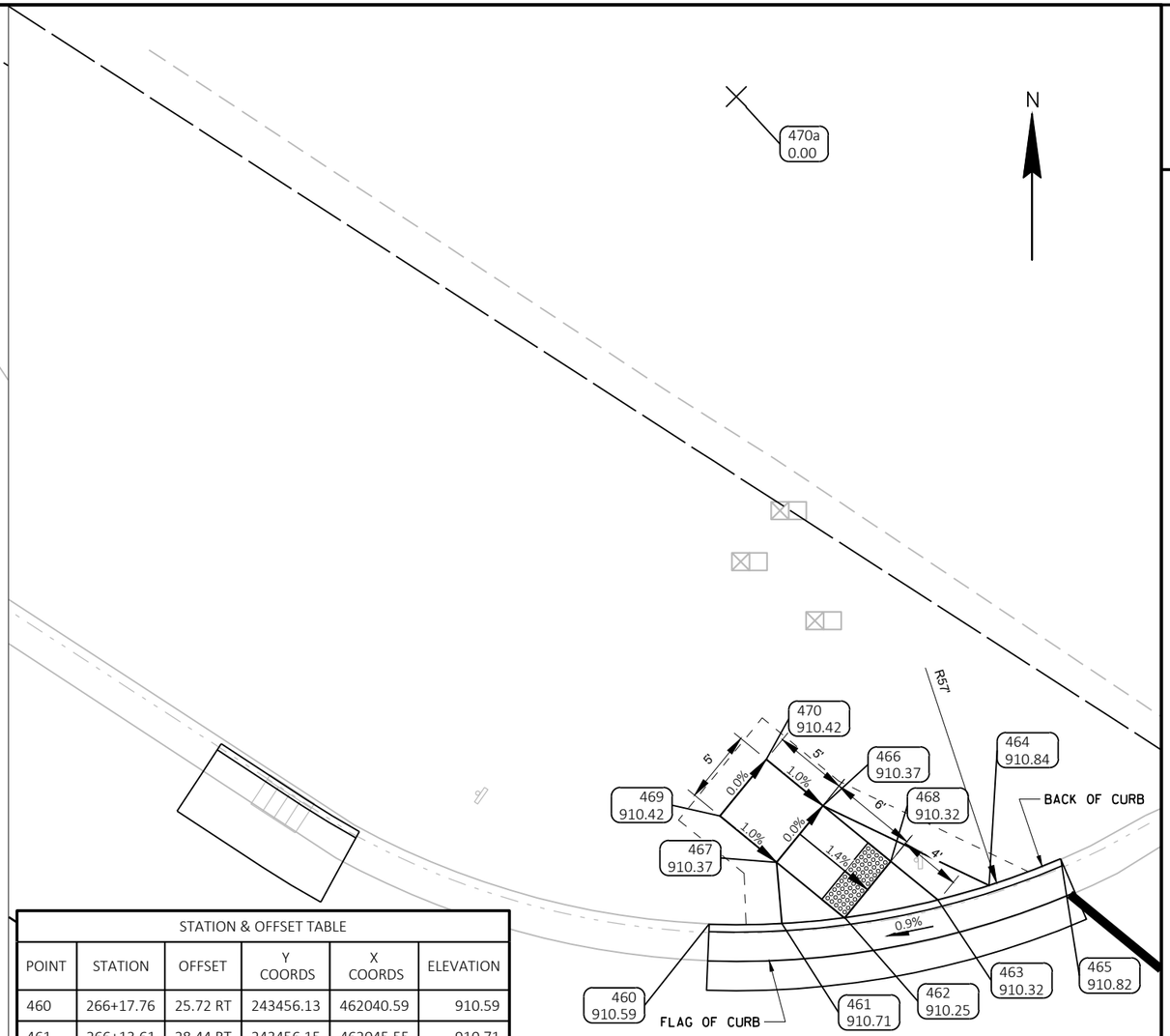
WISDOT/CADDS SHEET 42



STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
420	264+53.69	24.79 RT	243352.44	462173.15	910.20
421	264+53.42	29.74 RT	243355.84	462176.75	910.39
422	264+69.88	25.48 RT	243364.25	462161.36	909.48
423	264+69.62	30.47 RT	243367.77	462164.92	909.53
424	264+74.53	30.50 RT	243371.20	462161.15	909.53
425	264+74.74	25.55 RT	243367.65	462157.70	909.52
426	264+88.41	30.24 RT	243380.51	462150.19	910.50
427	264+88.55	25.33 RT	243376.89	462146.87	910.34
428	264+62.50	16.75 RT	243352.70	462161.07	909.74
429	264+67.25	16.97 RT	243356.13	462157.65	909.84
430	264+70.19	17.00 RT	243358.18	462155.45	909.35
431	264+75.03	17.08 RT	243361.54	462151.82	909.39
432	264+77.99	17.14 RT	243363.59	462149.61	909.90
433	264+82.82	17.10 RT	243366.82	462145.89	909.87

N HARVEY ST. & STH 80
MID BLOCK
CURB RAMP TYPE 7B MID BLOCK CROSSING-PAVED FLARES

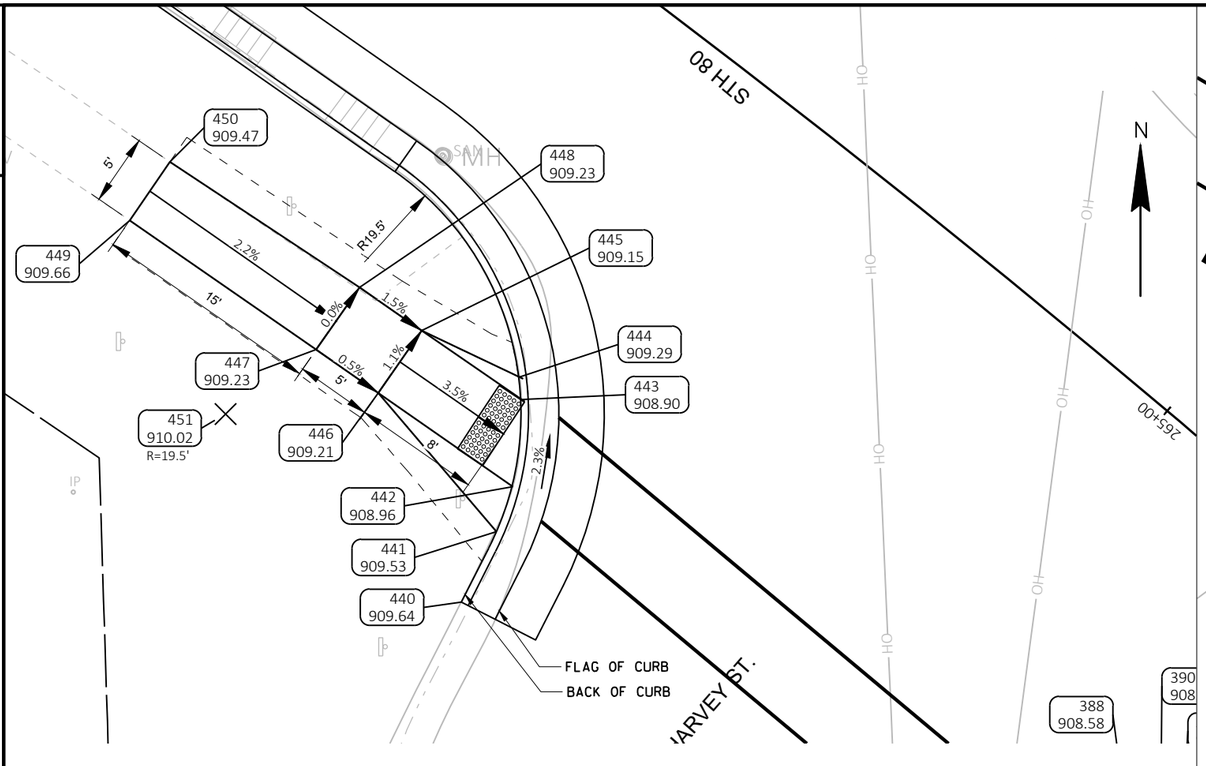


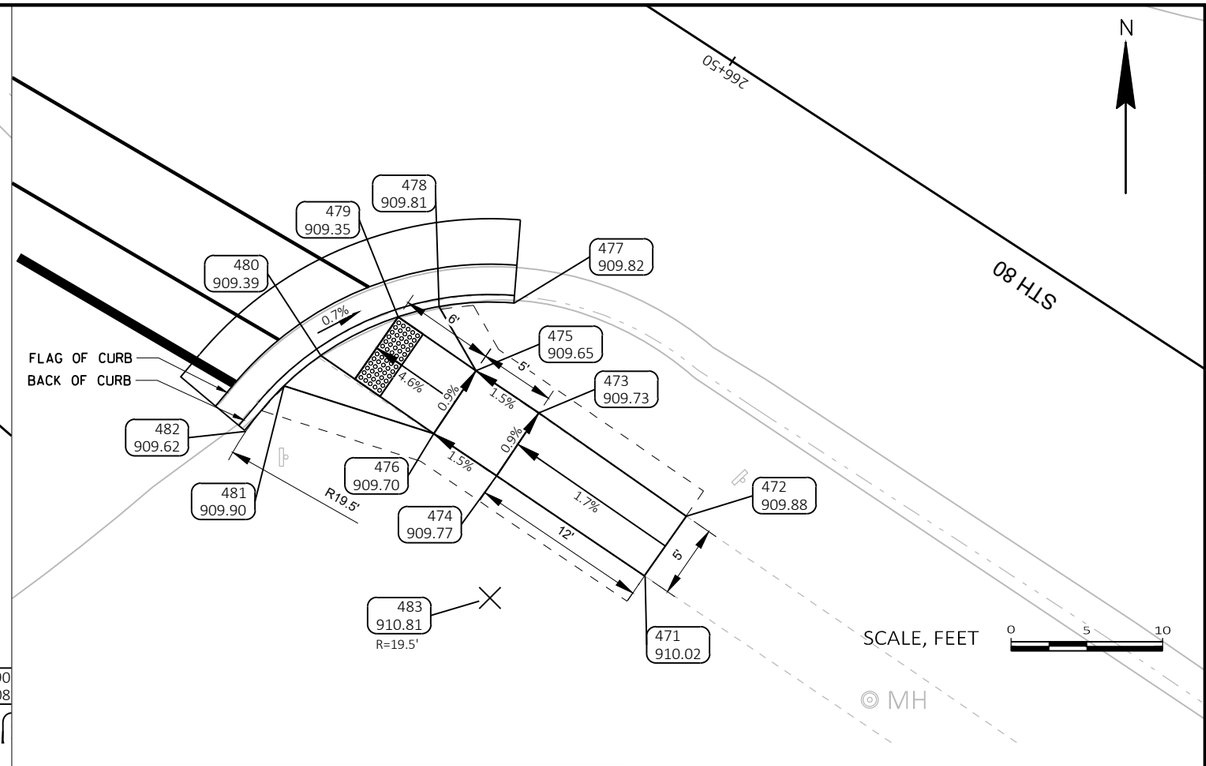
STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
460	266+17.76	25.72 RT	243456.13	462040.59	910.59
461	266+13.61	28.44 RT	243456.15	462045.55	910.71
462	266+10.22	31.12 RT	243456.55	462049.85	910.25
463	266+05.55	35.61 RT	243457.76	462056.22	910.32
464	266+03.18	38.34 RT	243458.76	462059.70	910.84
465	266+00.19	42.51 RT	243460.53	462064.60	910.82
466	266+15.64	36.71 RT	243464.19	462048.36	910.37
467	266+16.19	31.74 RT	243460.33	462045.19	910.37
468	266+09.68	36.06 RT	243460.39	462053.00	910.32
469	266+21.16	32.28 RT	243463.49	462041.32	910.42
470A			243512.59	462042.45	0.00
470	266+20.61	37.25 RT	243467.36	462044.49	910.42

SCALE, FEET 0 5 10

N HARVEY ST. & STH 80
NORTH QUADRANT
CURB RAMP TYPE 4B1-PAVED FLARES

PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	CURB RAMP DETAILS	SHEET 25	E





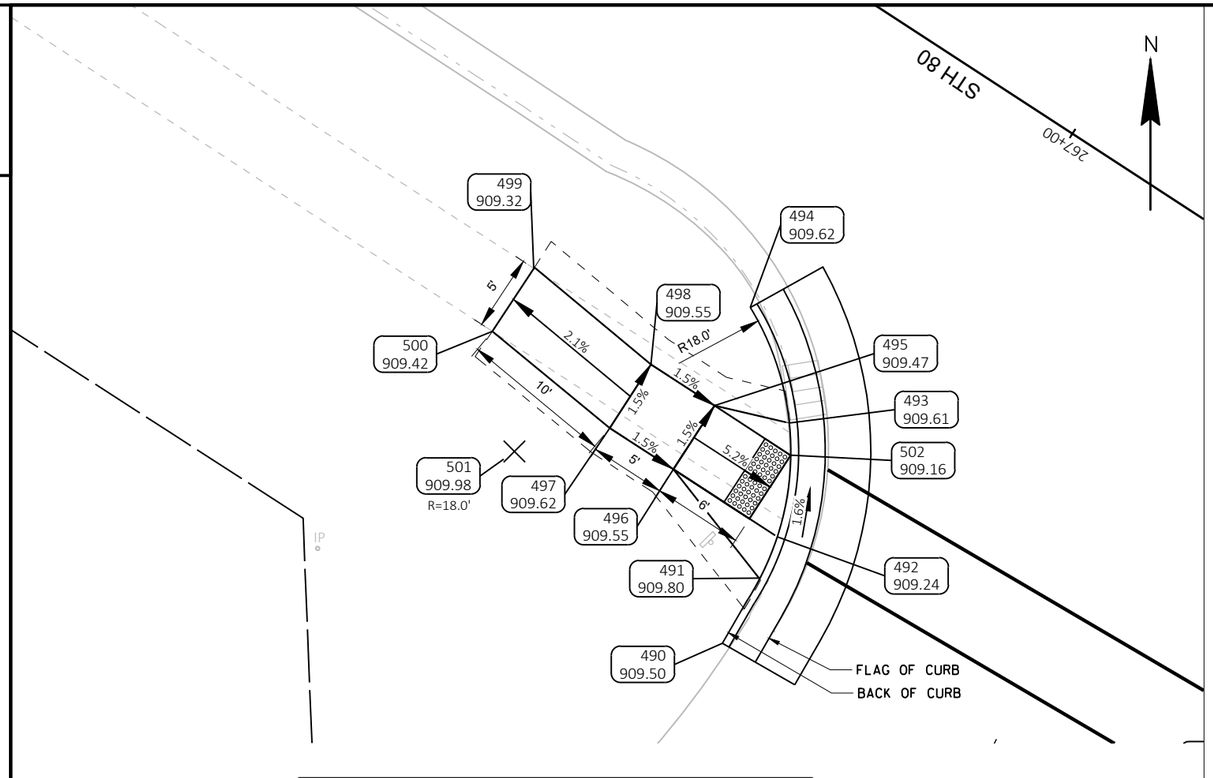
STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
440	265+28.87	-39.19 LT	243352.56	462074.99	909.64
441	265+29.96	-34.10 LT	243357.21	462077.31	909.53
442	265+30.98	-31.09 LT	243360.18	462078.38	908.96
443	265+34.20	-26.25 LT	243365.92	462078.90	908.90
444	265+35.21	-25.12 LT	243367.41	462078.82	909.29
445	265+42.41	-26.61 LT	243370.46	462072.37	909.15
446	265+42.20	-31.61 LT	243366.36	462069.51	909.21
447	265+47.39	-31.80 LT	243369.22	462065.41	909.23
448	265+47.57	-26.80 LT	243373.32	462068.27	909.23
449	265+62.94	-32.23 LT	243377.74	462053.11	909.66
450	265+63.05	-27.55 LT	243381.60	462055.76	909.47
451	265+49.74	-38.79 LT	243364.97	462059.43	910.02

N HARVEY ST. & STH 80
SW QUADRANT
CURB RAMP TYPE 4B1-PAVED FLARES

STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
471	266+36.29	-31.62 LT	243418.19	461993.79	910.02
472	266+36.17	-26.81 LT	243422.15	461996.50	909.88
473	266+48.04	-26.40 LT	243428.97	461986.78	909.73
474	266+48.11	-31.40 LT	243424.82	461983.99	909.77
475	266+53.04	-26.34 LT	243431.76	461982.62	909.65
476	266+53.11	-31.34 LT	243427.60	461979.84	909.70
477	266+53.38	-21.25 LT	243436.20	461985.12	909.82
478	266+57.38	-24.12 LT	243435.98	461980.20	909.81
479	266+59.27	-26.16 LT	243435.30	461977.50	909.35
480	266+62.16	-31.08 LT	243432.76	461972.39	909.39
481	266+63.10	-34.08 LT	243430.75	461969.97	909.90
482	266+63.58	-37.98 LT	243427.75	461967.44	909.62
483	266+44.09	-38.39 LT	243416.77	461983.56	910.81

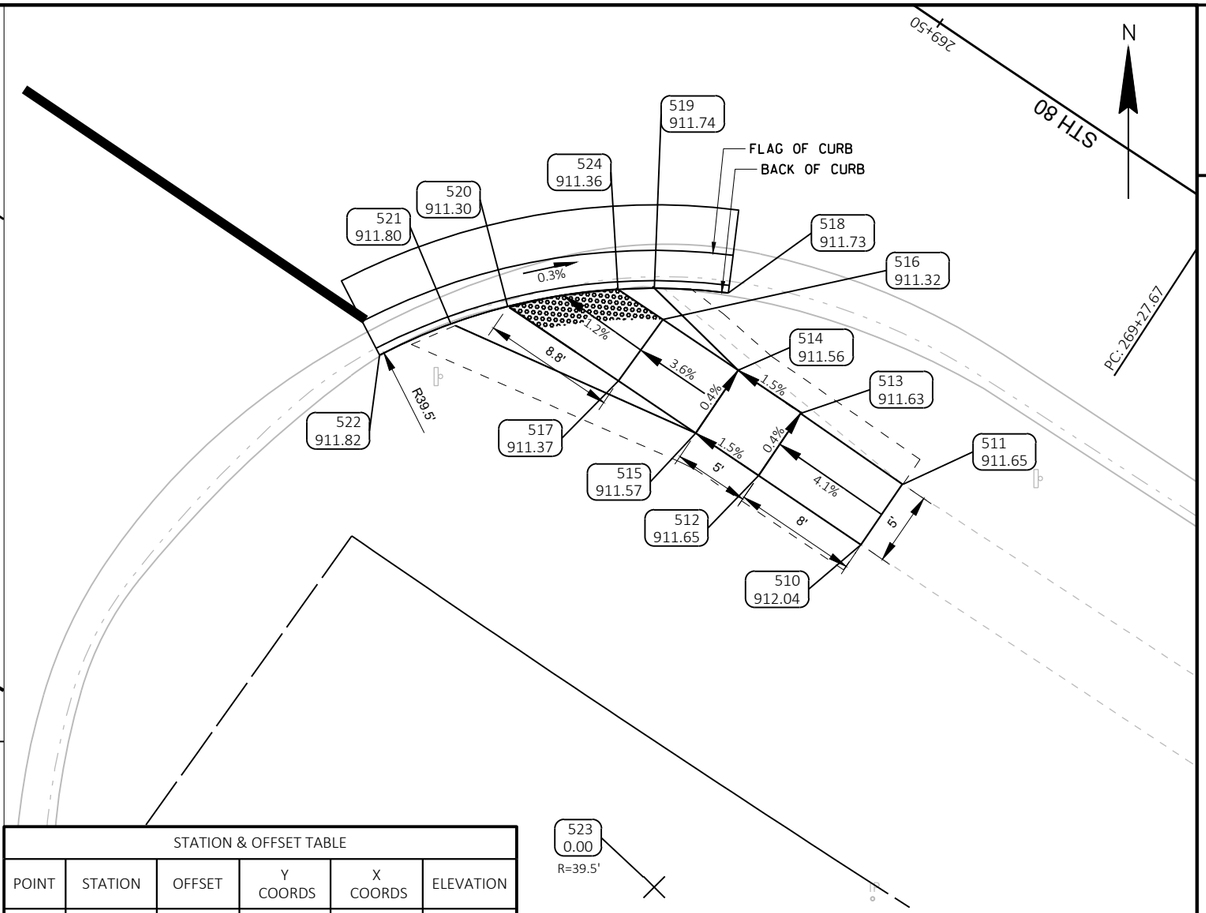
NORTH ST. & STH 80
SE QUADRANT
CURB RAMP TYPE 4B1-PAVED FLARES

PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	CURB RAMP DETAILS	SHEET 26	E



STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
490	267+01.15	-38.42 LT	243445.83	461934.54	909.50
491	267+01.35	-35.86 LT	243450.12	461936.95	909.80
492	267+01.79	-32.92 LT	243452.83	461938.18	909.24
493	267+05.25	-26.21 LT	243460.34	461938.94	909.61
494	267+11.57	-21.22 LT	243467.97	461936.37	909.62
495	267+10.01	-27.93 LT	243461.49	461934.01	909.47
496	267+10.00	-32.93 LT	243457.30	461931.29	909.55
497	267+15.00	-32.94 LT	243460.01	461927.10	909.62
498	267+15.01	-27.94 LT	243464.21	461929.82	909.55
499	267+24.94	-26.84 LT	243470.55	461922.09	909.32
500	267+24.97	-31.82 LT	243466.40	461919.35	909.42
501	267+19.42	-37.66 LT	243458.47	461920.82	909.98
502	267+04.06	-27.94 LT	243458.24	461938.99	909.16

NORTH ST. & STH 80
SW QUADRANT
CURB RAMP TYPE 4B1-PAVED FLARES



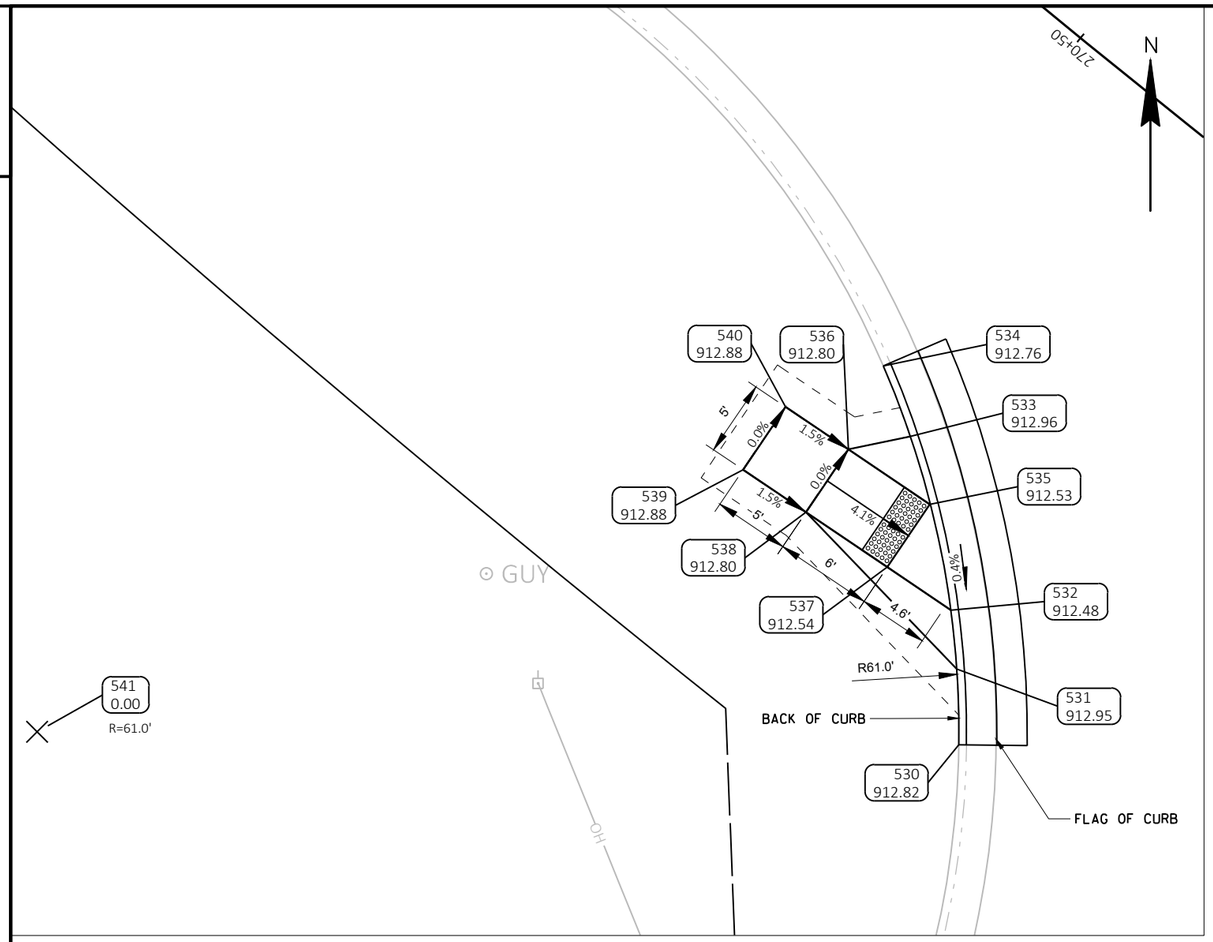
STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
510	269+35.38	-31.49 LT	243581.59	461743.02	912.04
511	269+35.28	-26.65 LT	243585.57	461745.78	911.65
512	269+43.32	-31.44 LT	243586.15	461736.26	911.65
513	269+43.31	-26.49 LT	243590.26	461739.03	911.63
514	269+48.19	-26.43 LT	243593.09	461734.91	911.56
515	269+48.19	-31.43 LT	243588.95	461732.11	911.57
516	269+54.06	-26.46 LT	243596.45	461729.94	911.32
517	269+54.18	-31.46 LT	243592.40	461727.01	911.37
518	269+51.53	-22.57 LT	243598.21	461734.26	911.73
519	269+55.65	-25.08 LT	243598.51	461729.37	911.74
520	269+62.77	-31.54 LT	243597.33	461719.70	911.30
521	269+65.10	-34.65 LT	243596.16	461715.95	911.80
522	269+67.69	-39.03 LT	243594.11	461711.25	911.82
523	269+34.38	-57.85 LT	243559.01	461729.37	0.00
524	269+57.53	-26.49 LT	243598.43	461726.99	911.36

RADIAL TILE									
WARNING FIELD AREA (SF)	RADIUS AT BACK OF CURB (FT)	LONG CHORD				LANDING DISTANCE			
		POINT	-	POINT	LENGTH	POINT	-	POINT	LENGTH (FT)
15.09	40	520	-	524	7.39	517	-	520	8.8

SCALE, FEET 0 5 10

JOHN ST. & STH 80
SE QUADRANT
CURB RAMP TYPE 4B1-PAVED FLARES

PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	CURB RAMP DETAILS	SHEET 27	E

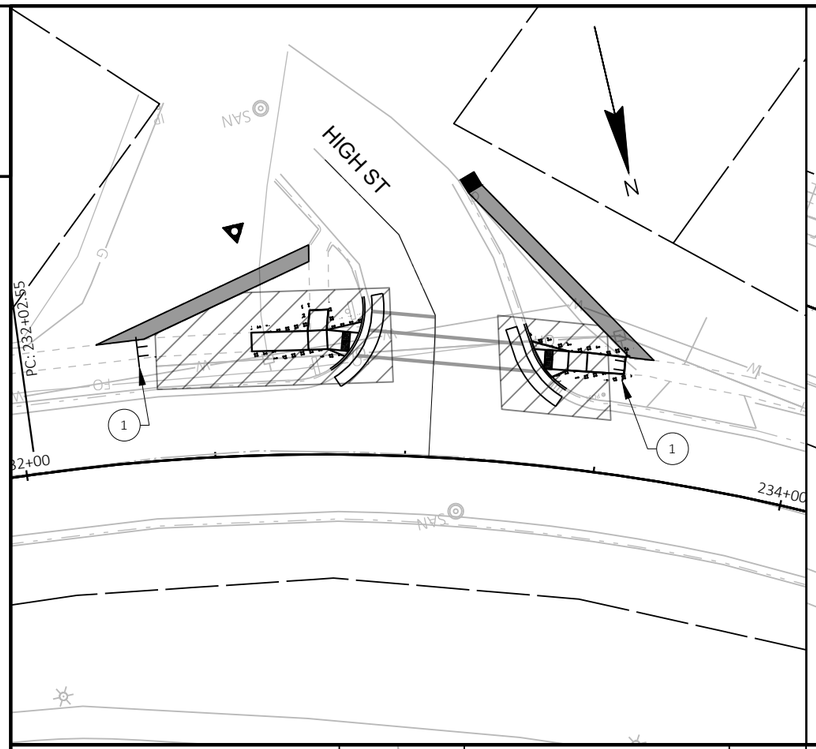


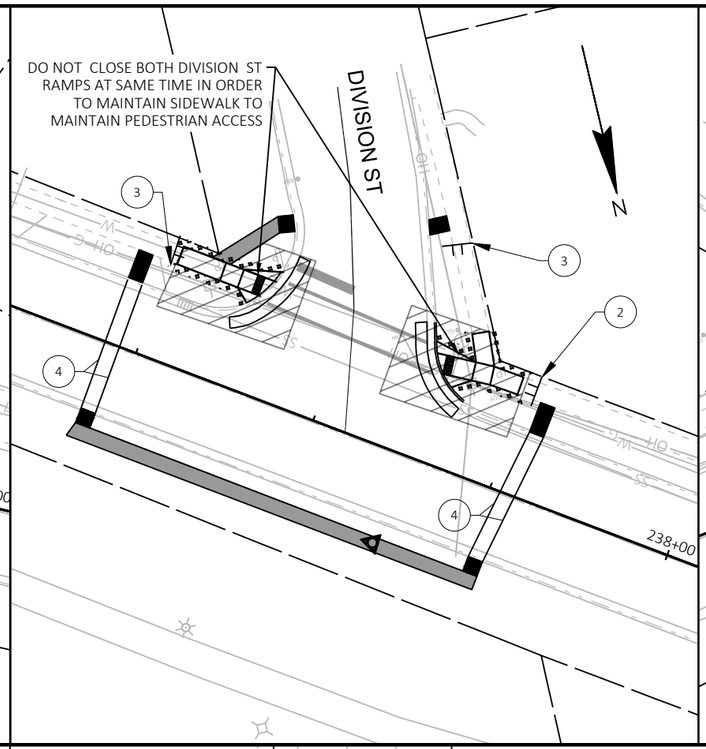
STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
530	270+27.57	-41.45 LT	243629.08	461660.03	912.82
531	270+30.67	-37.59 LT	243634.09	461659.89	912.95
532	270+33.30	-34.78 LT	243637.97	461659.50	912.48
533	270+42.29	-27.49 LT	243649.43	461656.83	912.96
534	270+46.49	-25.00 LT	243654.07	461655.04	912.76
535	270+38.59	-30.19 LT	243644.96	461658.10	912.53
536	270+44.87	-30.73 LT	243648.57	461652.73	912.80
537	270+38.20	-35.18 LT	243640.81	461655.30	912.54
538	270+44.45	-35.71 LT	243644.43	461649.94	912.80
539	270+49.28	-36.14 LT	243647.23	461645.79	912.88
540	270+49.72	-31.16 LT	243651.37	461648.59	912.88
541	270+72.88	-79.22 LT	243629.94	461599.20	0.00

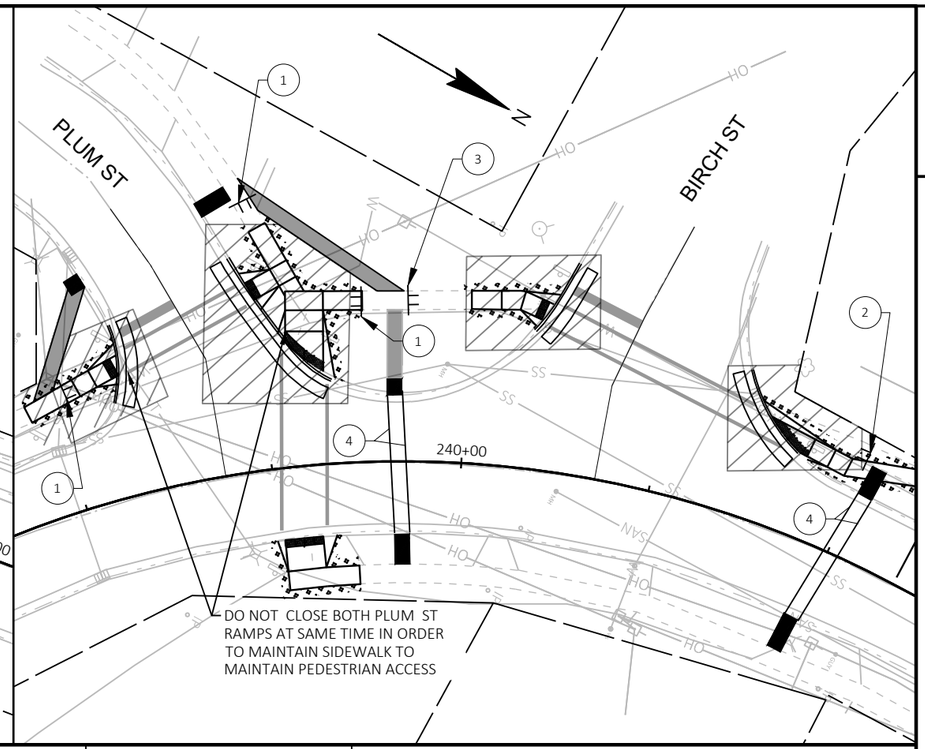
SCALE, FEET 0 5 10

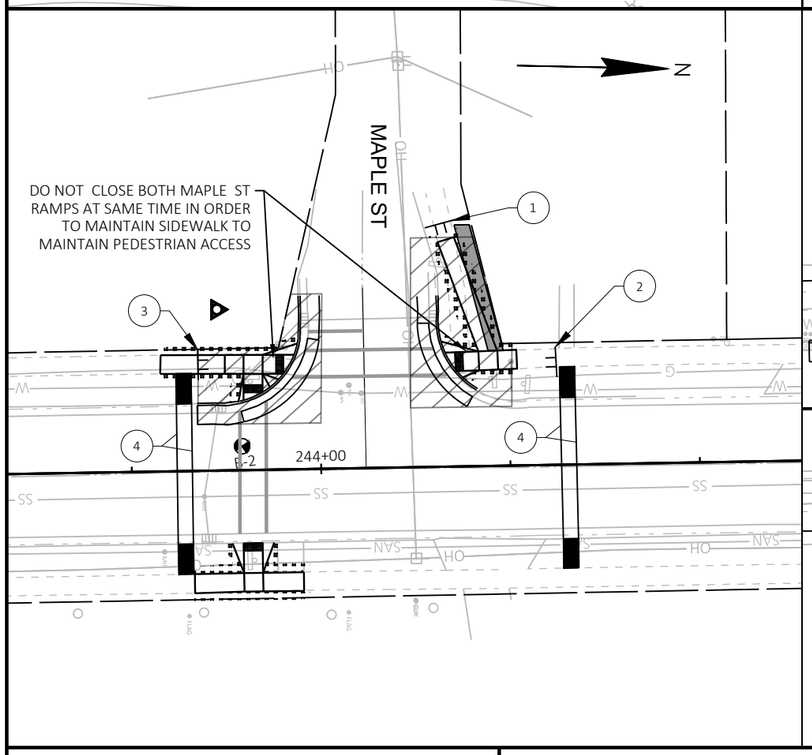
JOHN ST. & STH 80
SW QUADRANT
CURB RAMP TYPE 4B1-PAVED FLARES

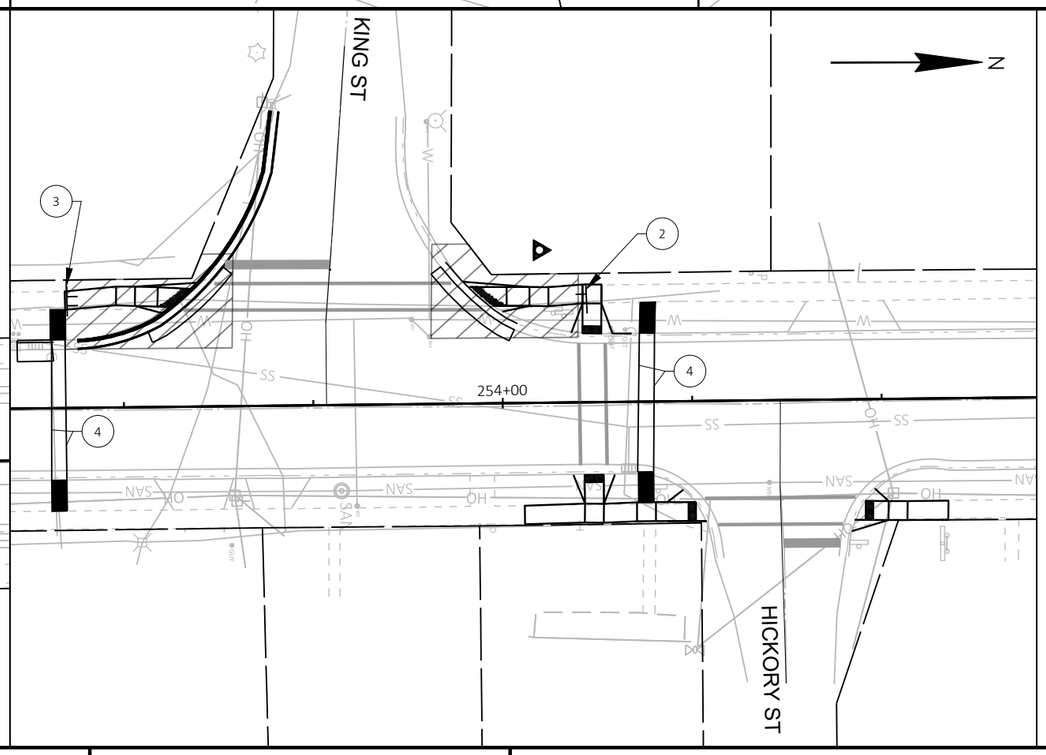
PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	CURB RAMP DETAILS	SHEET 28	E











NOTES:

1. SIDEWALK TO BE CLOSED HALF AT A TIME. STH 80 TO REMAIN OPEN FOR PEDESTRIAN TRAFFIC. USE TEMPORARY PEDESTRIAN SURFACE ITEMS, TEMPORARY CURB RAMPS, TEMPORARY PEDESTRIAN BARRICADE, AND TEMPORARY MARKING PAINT AS SPECIFIED. SEE SDD 15D30 TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION.
2. EXACT LOCATIONS TO BE DETERMINED BY ENGINEER IN FIELD.

LEGEND

- TEMPORARY CURB RAMPS
- TEMPORARY PEDESTRIAN SURFACE
- WORK AREA

1 — SIDEWALK CLOSED — R9-9 24"X12"

2 — SIDEWALK CLOSED CROSS HERE — R9-11A 24"X12"

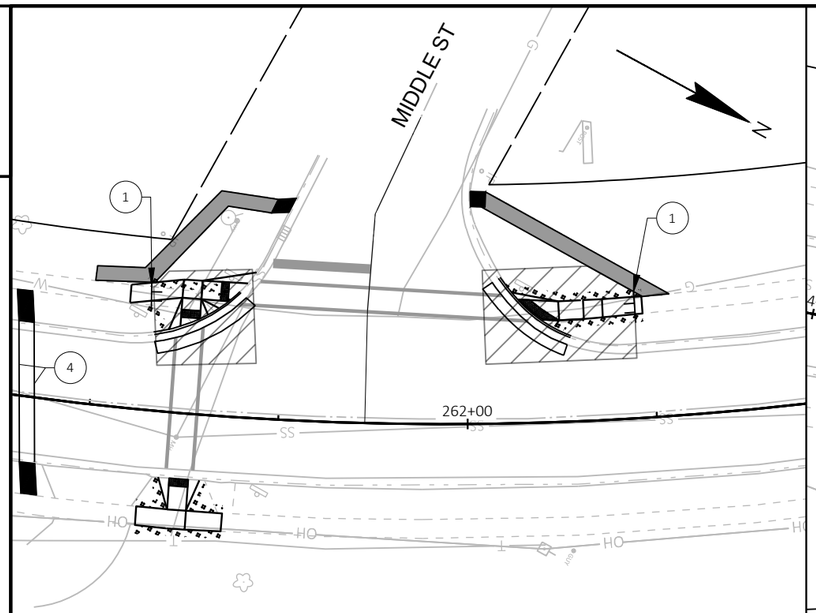
3 — SIDEWALK CLOSED CROSS HERE — R9-11A 24"X12"

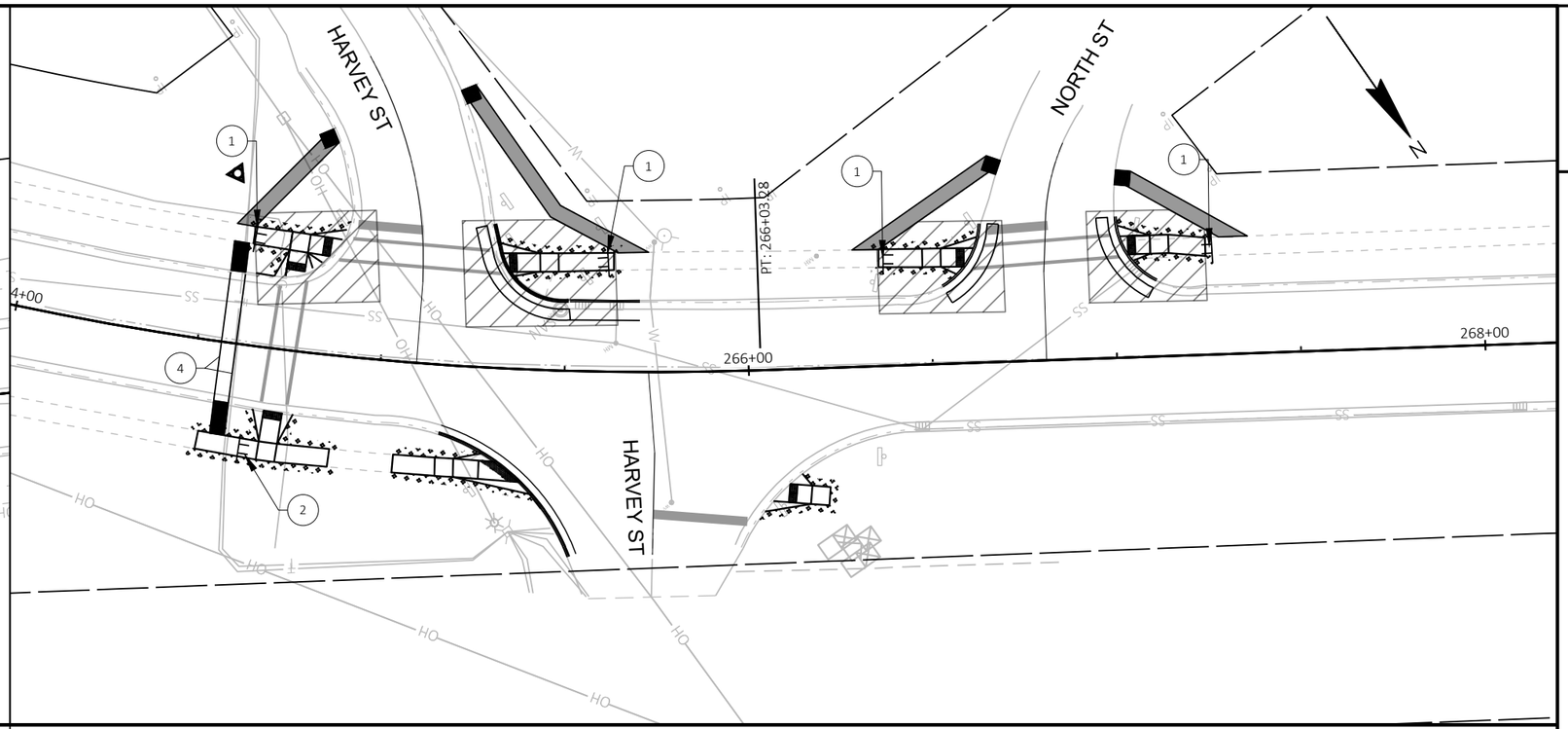
4 — TEMPORARY MARKING LINE PAINT 4-INCH FOR TEMPORARY CROSSINGS

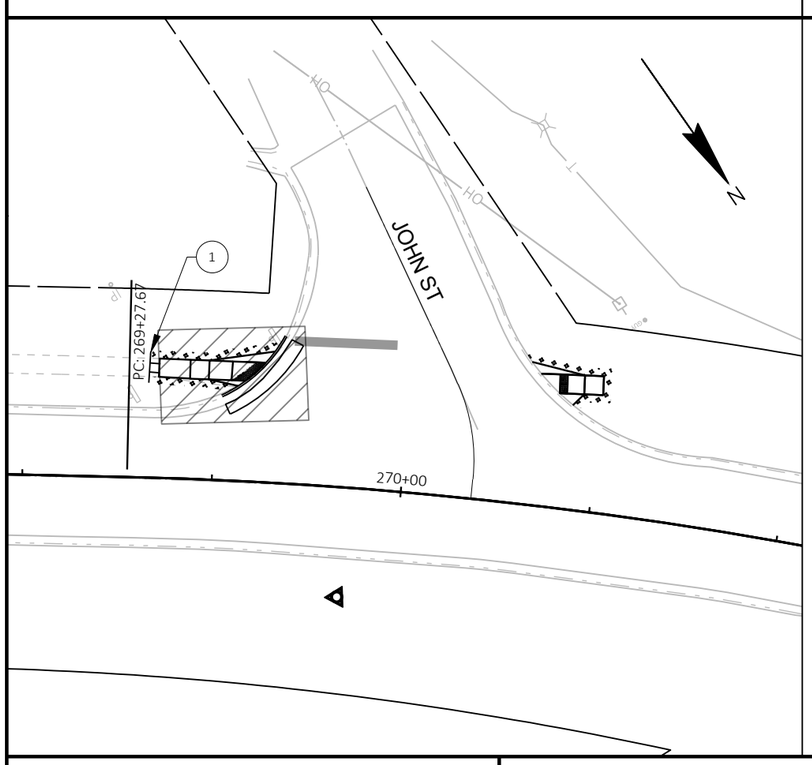
SCALE, FEET 0 20 40

PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	TRAFFIC CONTROL PLAN - PEDESTRIAN STAGE 1 (WEST SIDE CONSTRUCTION)	SHEET 29	E

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\025100_TC WEST.DWG
LAYOUT NAME - 1
PLOT DATE : 5/27/2022 10:23 AM
PLOT BY : MARZOLF, ROBBI
PLOT NAME :
PLOT SCALE : ##########
WISDOT/CADDS SHEET 44







NOTES:

1. SIDEWALK TO BE CLOSED HALF AT A TIME. STH 80 TO REMAIN OPEN FOR PEDESTRIAN TRAFFIC. USE TEMPORARY PEDESTRIAN SURFACE ITEMS, TEMPORARY CURB RAMPS, TEMPORARY PEDESTRIAN BARRICADE, AND TEMPORARY MARKING PAINT AS SPECIFIED. SEE SDD 15D30 TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION.
2. EXACT LOCATIONS TO BE DETERMINED BY ENGINEER IN FIELD.

LEGEND

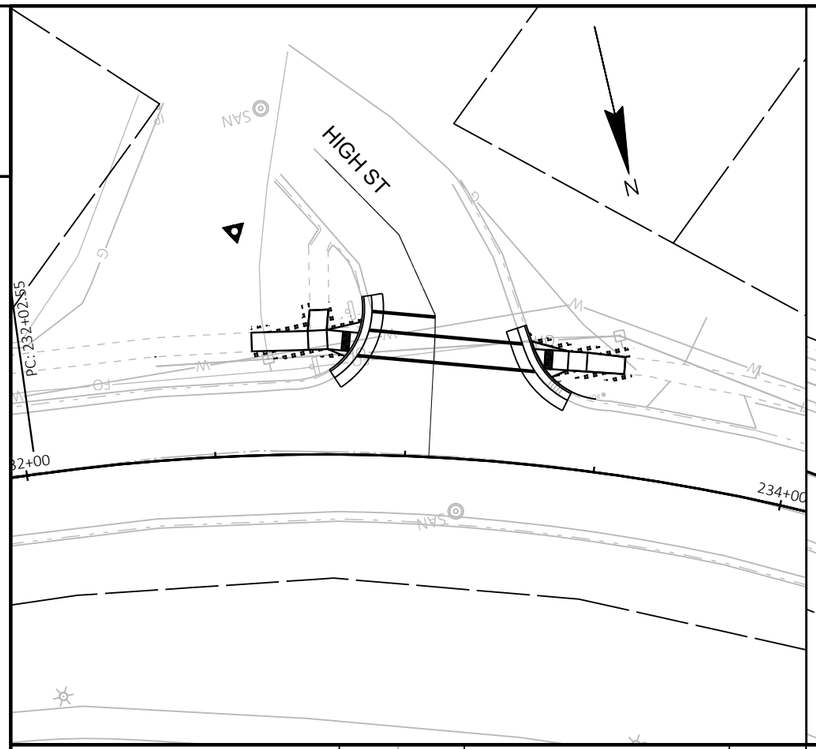
- TEMPORARY CURB RAMPS
- TEMPORARY PEDESTRIAN SURFACE
- WORK AREA

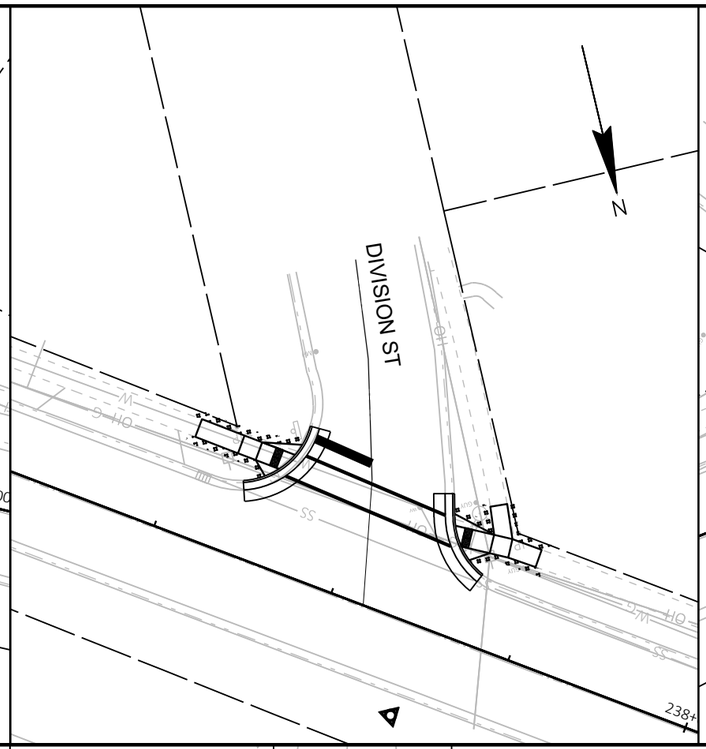
1. SIDEWALK CLOSED — R9-9 — 24"X12"
2. SIDEWALK CLOSED CROSS HERE — R9-11A — 24"X12"
3. SIDEWALK CLOSED CROSS HERE — R9-11A — 24"X12"
4. TEMPORARY MARKING LINE PAINT 4-INCH FOR TEMPORARY CROSSINGS

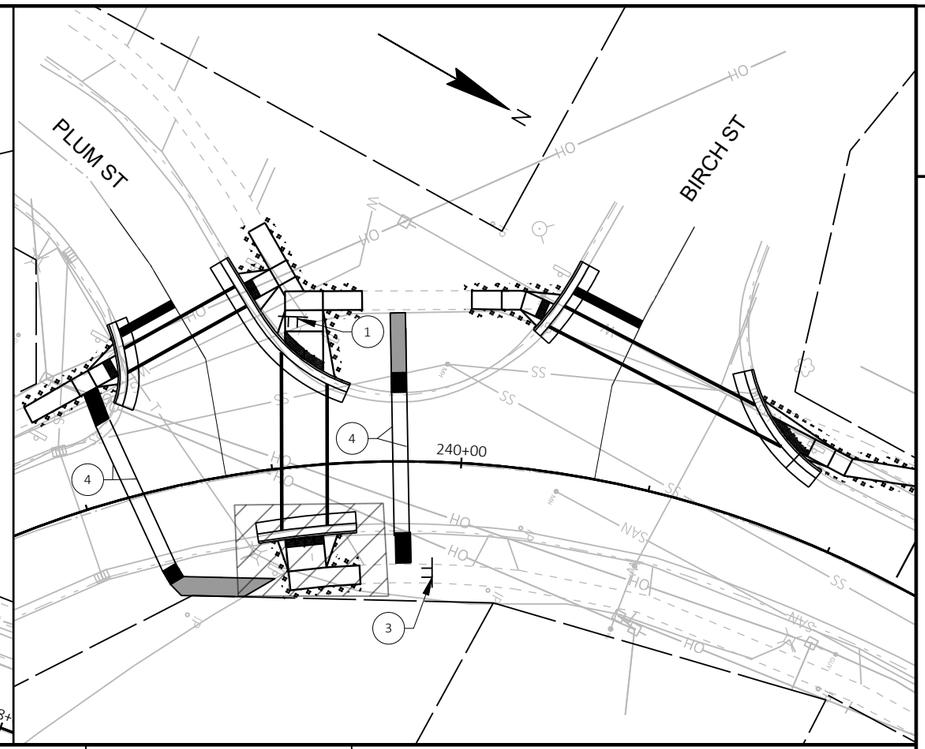
SCALE, FEET 0 20 40

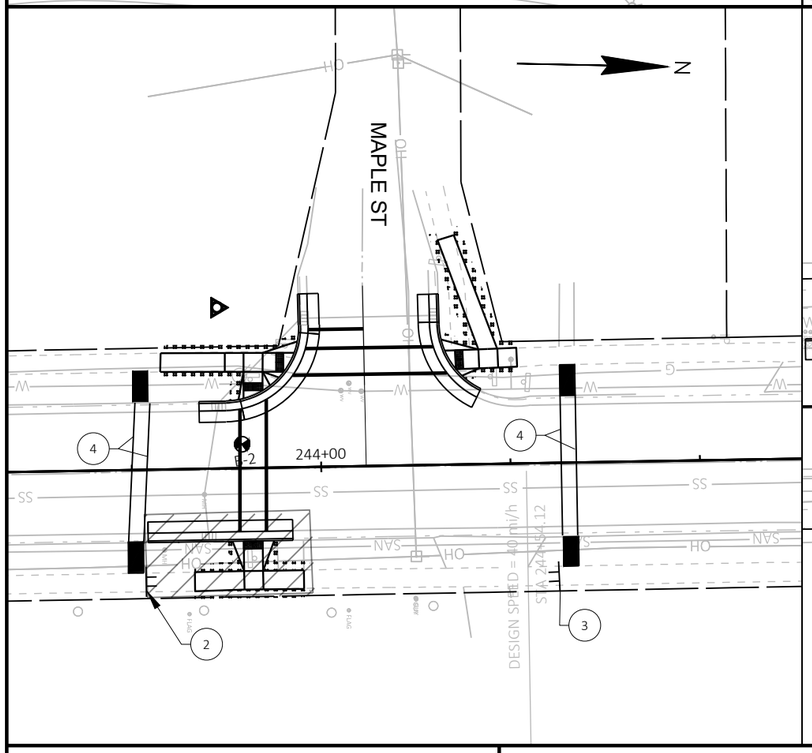
PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	TRAFFIC CONTROL PLAN - PEDESTRIAN STAGE 1 (WEST SIDE CONSTRUCTION)	SHEET 30	E

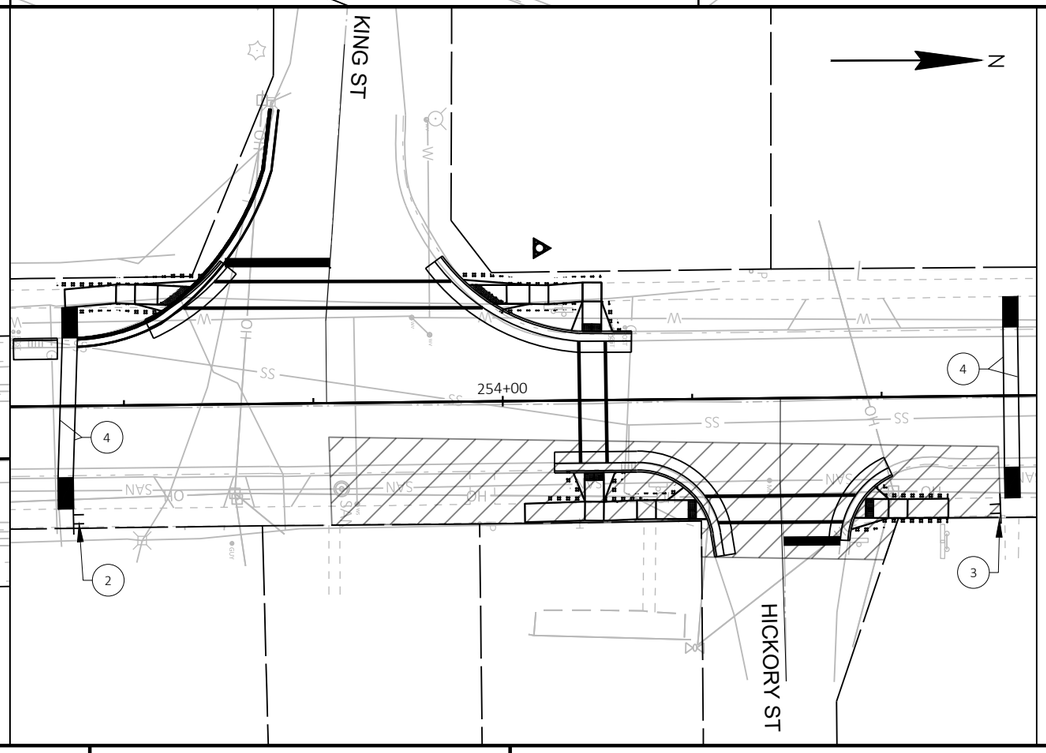
FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\025100_TC WEST.DWG
LAYOUT NAME - 2
PLOT DATE : 5/27/2022 10:24 AM PLOT BY : MARZOLF, ROBBI PLOT NAME : PLOT SCALE : ##########
WISDOT/CADDS SHEET 44











NOTES:

1. SIDEWALK TO BE CLOSED HALF AT A TIME. STH 80 TO REMAIN OPEN FOR PEDESTRIAN TRAFFIC. USE TEMPORARY PEDESTRIAN SURFACE ITEMS, TEMPORARY CURB RAMPS, TEMPORARY PEDESTRIAN BARRICADE, AND TEMPORARY MARKING PAINT AS SPECIFIED. SEE SDD 15D30 TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION.
2. EXACT LOCATIONS TO BE DETERMINED BY ENGINEER IN FIELD.

LEGEND

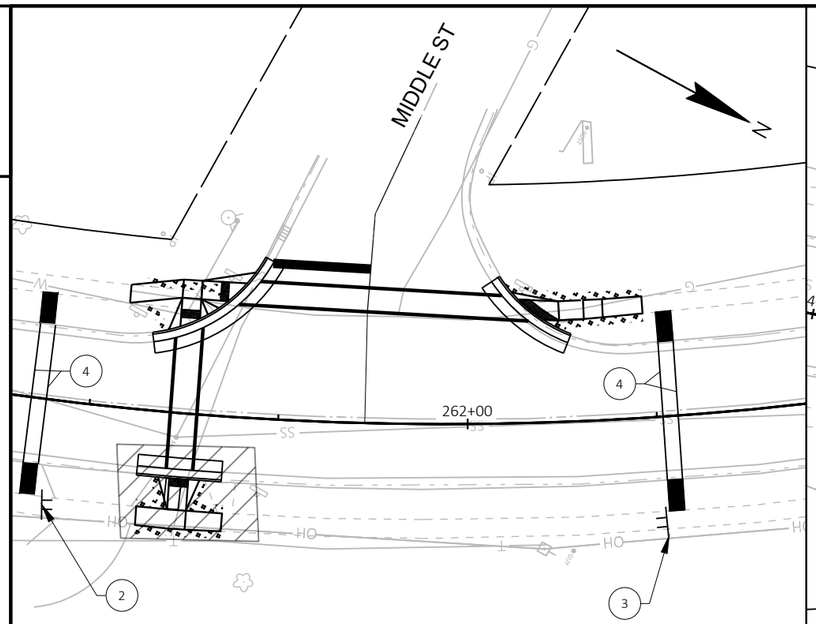
- TEMPORARY CURB RAMPS
- TEMPORARY PEDESTRIAN SURFACE
- WORK AREA

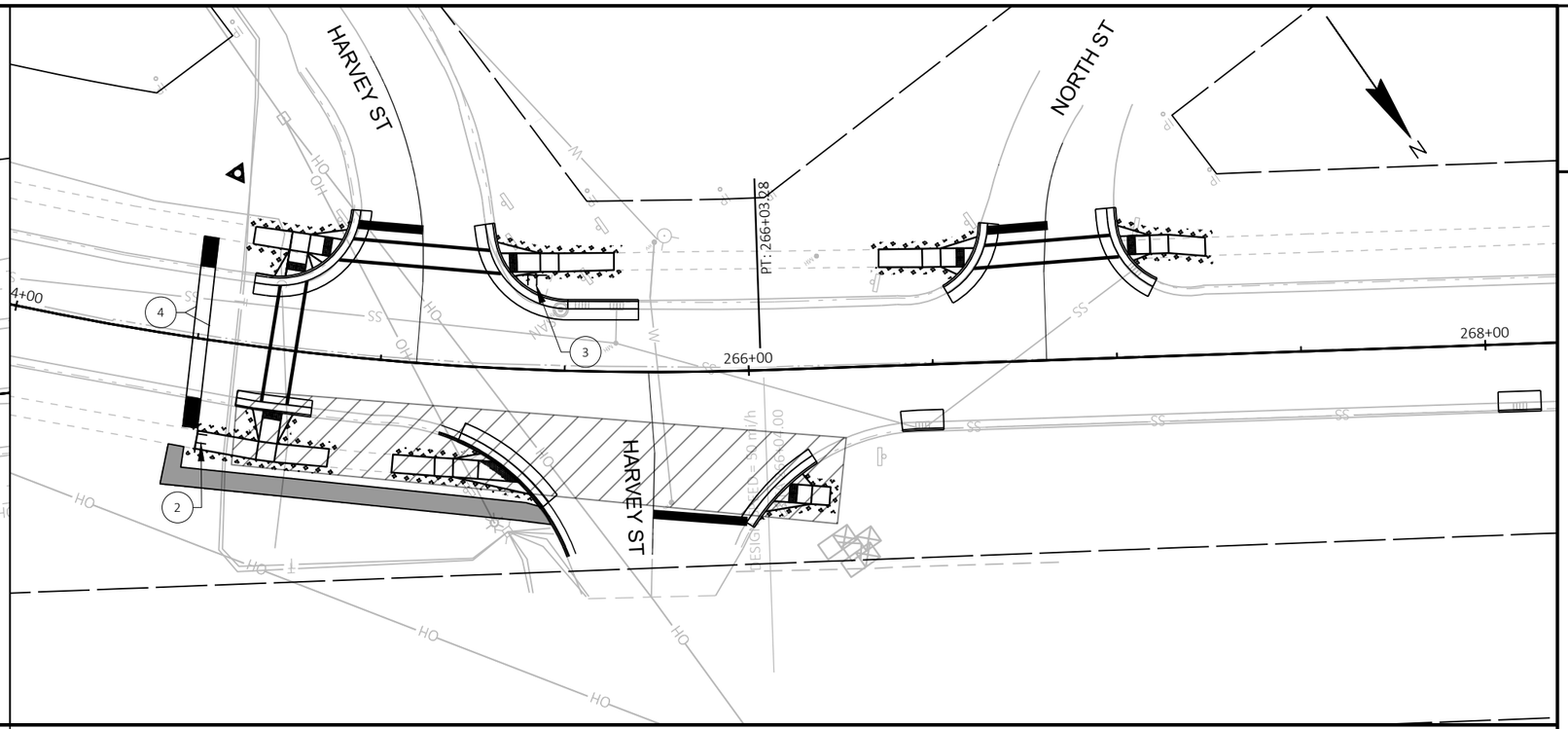
1. SIDEWALK CLOSED R9-9 24"X12"
2. SIDEWALK CLOSED CROSS HERE R9-11A 24"X12"
3. SIDEWALK CLOSED CROSS HERE R9-11A 24"X12"
4. TEMPORARY MARKING LINE PAINT 4-INCH FOR TEMPORARY CROSSINGS

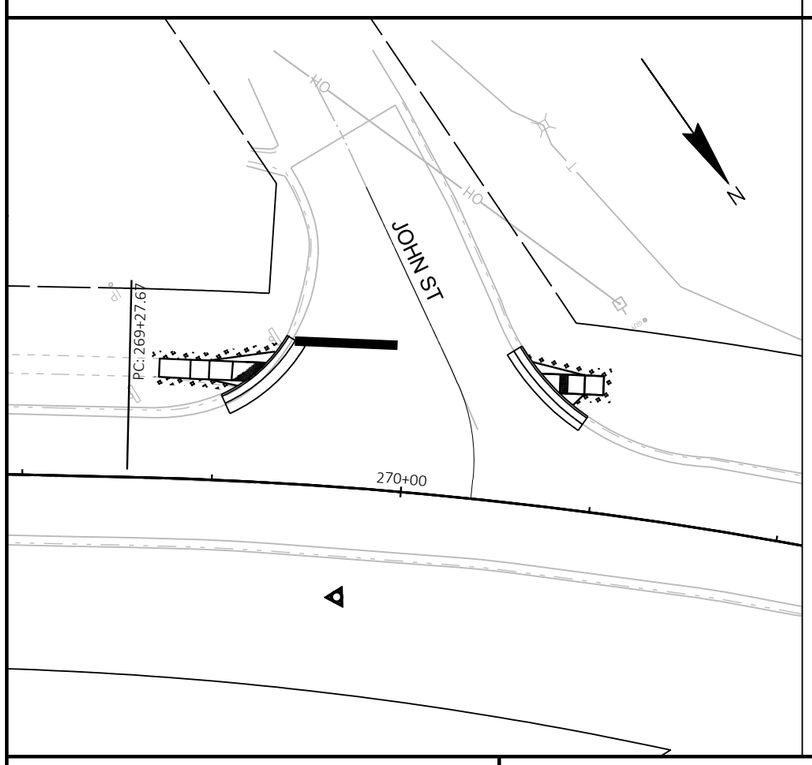
SCALE, FEET 0 20 40

PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	TRAFFIC CONTROL PLAN - PEDESTRIAN STAGE 2 (EAST SIDE CONSTRUCTION)	SHEET 31	E

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\025100_TC EAST.DWG
LAYOUT NAME - 1
PLOT DATE : 5/27/2022 10:20 AM
PLOT BY : MARZOLF, ROBBI
PLOT NAME :
PLOT SCALE : ##########
WISDOT/CADDS SHEET 44







NOTES:

1. SIDEWALK TO BE CLOSED HALF AT A TIME. STH 80 TO REMAIN OPEN FOR PEDESTRIAN TRAFFIC. USE TEMPORARY PEDESTRIAN SURFACE ITEMS, TEMPORARY CURB RAMPS, TEMPORARY PEDESTRIAN BARRICADE, AND TEMPORARY MARKING PAINT AS SPECIFIED. SEE SDD 15D30 TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION.
2. EXACT LOCATIONS TO BE DETERMINED BY ENGINEER IN FIELD.

LEGEND

- TEMPORARY CURB RAMPS
- TEMPORARY PEDESTRIAN SURFACE
- WORK AREA

1. SIDEWALK CLOSED — R9-9 24"X12"
2. SIDEWALK CLOSED CROSS HERE — R9-11A 24"X12"
3. SIDEWALK CLOSED CROSS HERE — R9-11A 24"X12"
4. TEMPORARY MARKING LINE PAINT 4-INCH FOR TEMPORARY CROSSINGS

SCALE, FEET 0 20 40

PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	TRAFFIC CONTROL PLAN - PEDESTRIAN STAGE 2 (EAST SIDE CONSTRUCTION)	SHEET 32	E

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\025100_TC EAST.DWG
LAYOUT NAME - 2
PLOT DATE : 5/27/2022 10:20 AM PLOT BY : MARZOLF, ROBBI PLOT NAME : PLOT SCALE : ##########
WISDOT/CADDS SHEET 44

2

LEGEND

- POST WITH ATTACHED SIGN
- EXISTING SIGN
- DETOUR ROUTE
- TYPE III BARRICADE WITH/WITHOUT SIGN
- COVER EXISTING SIGN

NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE MUTCD. ADJUSTMENTS TO FIT FIELD CONDITIONS SHALL BE APPROVED BY THE ENGINEER.

"WO" SIGNS ARE THE SAME AS "W" SIGNS AND "MO" SIGNS ARE THE SAME AS "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

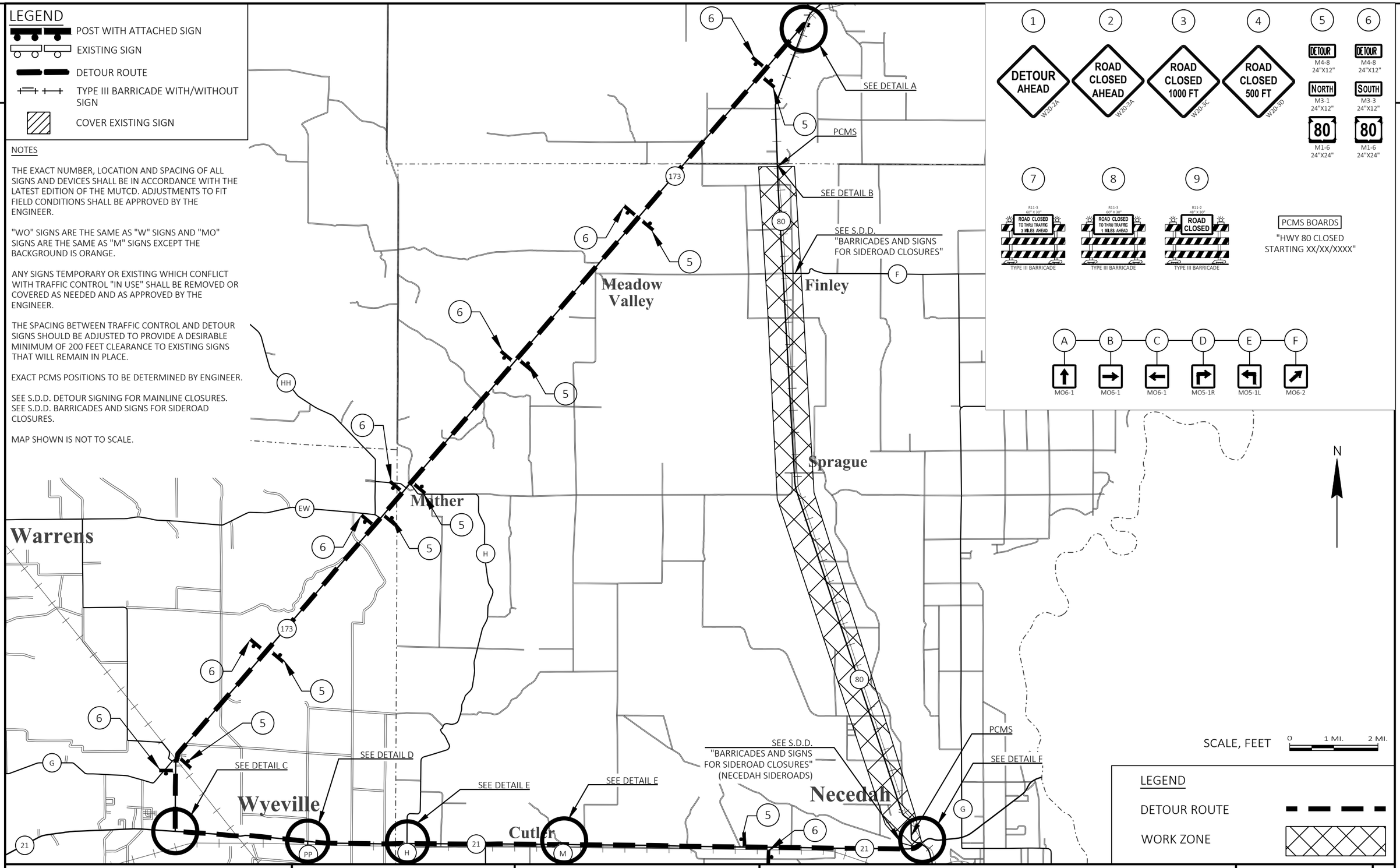
ANY SIGNS TEMPORARY OR EXISTING WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL AND DETOUR SIGNS SHOULD BE ADJUSTED TO PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

EXACT PCMS POSITIONS TO BE DETERMINED BY ENGINEER.

SEE S.D.D. DETOUR SIGNING FOR MAINLINE CLOSURES.
SEE S.D.D. BARRICADES AND SIGNS FOR SIDEROAD CLOSURES.

MAP SHOWN IS NOT TO SCALE.



1 DETOUR AHEAD W20-2A
2 ROAD CLOSED AHEAD W20-3A
3 ROAD CLOSED 1000 FT W20-3C
4 ROAD CLOSED 500 FT W20-3D
5 DETOUR M4-8 24"X12"; NORTH M3-1 24"X12"; 80 M1-6 24"X24"
6 DETOUR M4-8 24"X12"; SOUTH M3-3 24"X12"; 80 M1-6 24"X24"

7 R11-3 60" X 30" ROAD CLOSED TO THRU TRAFFIC 3 MILES AHEAD — TYPE III BARRICADE
8 R11-3 60" X 30" ROAD CLOSED TO THRU TRAFFIC 1 MILES AHEAD — TYPE III BARRICADE
9 R11-2 48" X 30" ROAD CLOSED — TYPE III BARRICADE

PCMS BOARDS
"HWY 80 CLOSED STARTING XX/XX/XXXX"

A MO6-1; B MO6-1; C MO6-1; D MO5-1R; E MO5-1L; F MO6-2

PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	DETOUR PLAN	SHEET 33	E

FILE NAME : W:\7607\CAD\1 1620-02-08\SHEETSPLAN\027001_DT.DWG
LAYOUT NAME - 1
PLOT DATE : 5/31/2022 9:22 AM
PLOT BY : MARZOLF, ROBBI
PLOT NAME :
PLOT SCALE : 1 IN:2 MI

2

LEGEND

- POST WITH ATTACHED SIGN
- EXISTING SIGN
- DETOUR ROUTE
- TYPE III BARRICADE WITH/WITHOUT SIGN
- COVER EXISTING SIGN

1 — DETOUR AHEAD — W20-2A

2 — ROAD CLOSED AHEAD — W20-3A

3 — ROAD CLOSED 1000 FT — W20-3C

4 — ROAD CLOSED 500 FT — W20-3D

5 — DETOUR M4-8 24"X12"; NORTH M3-1 24"X12"; 80 M1-6 24"X24"

6 — DETOUR M4-8 24"X12"; SOUTH M3-3 24"X12"; 80 M1-6 24"X24"

7 — ROAD CLOSED TO THRU TRAFFIC 3 MILES AHEAD — TYPE III BARRICADE

8 — ROAD CLOSED TO THRU TRAFFIC 1 MILES AHEAD — TYPE III BARRICADE

9 — ROAD CLOSED — TYPE III BARRICADE

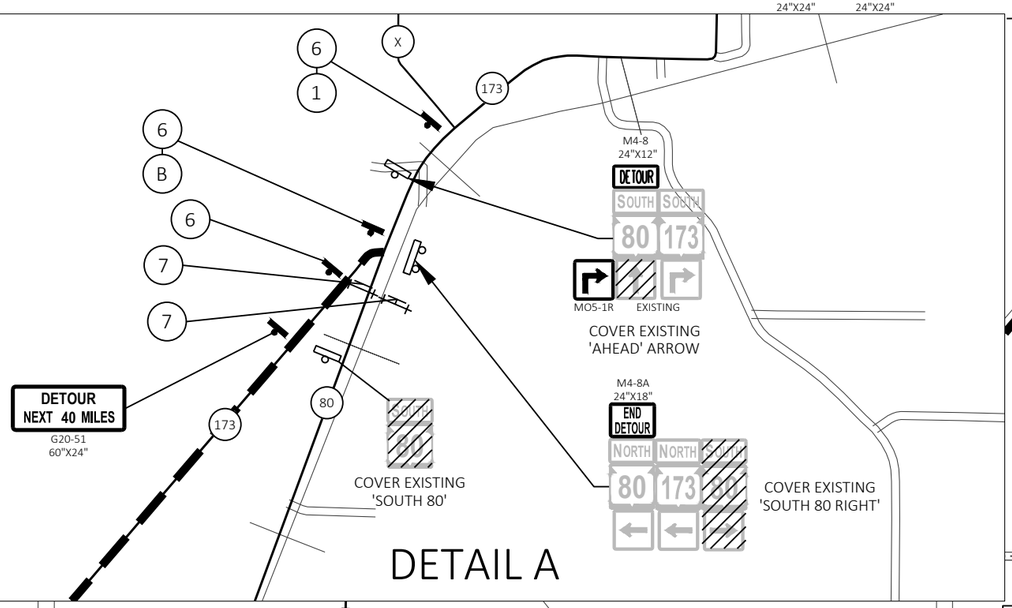
PCMS BOARDS

"HWY 80 CLOSED STARTING XX/XX/XXXX"

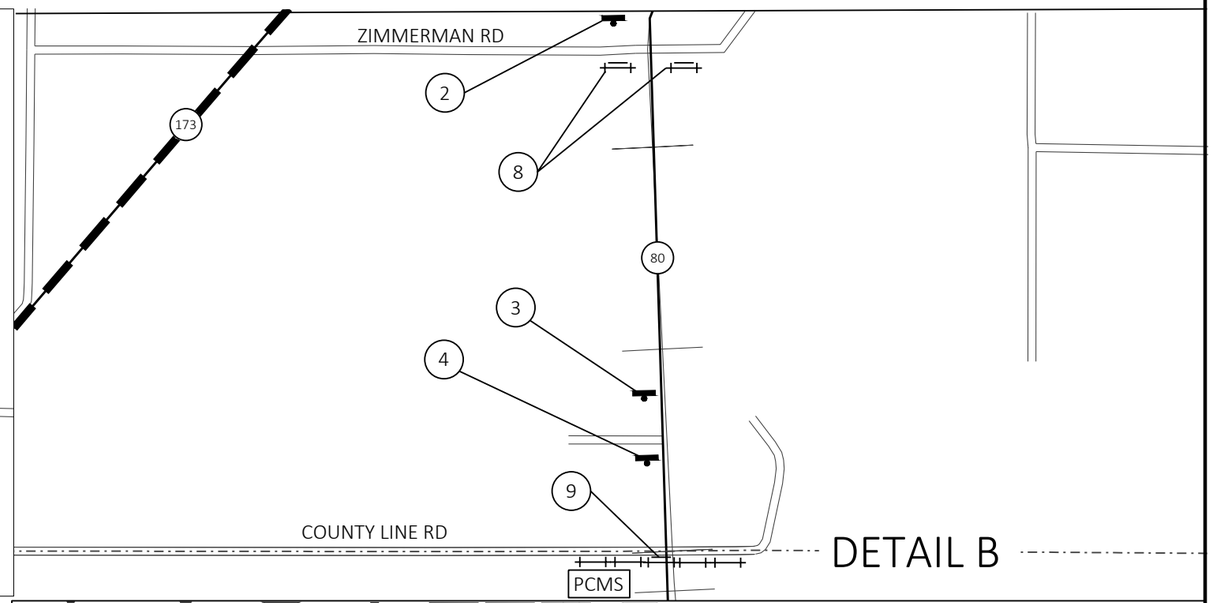
A MO6-1; B MO6-1; C MO6-1; D MO5-1R; E MO5-1L; F MO6-2

N

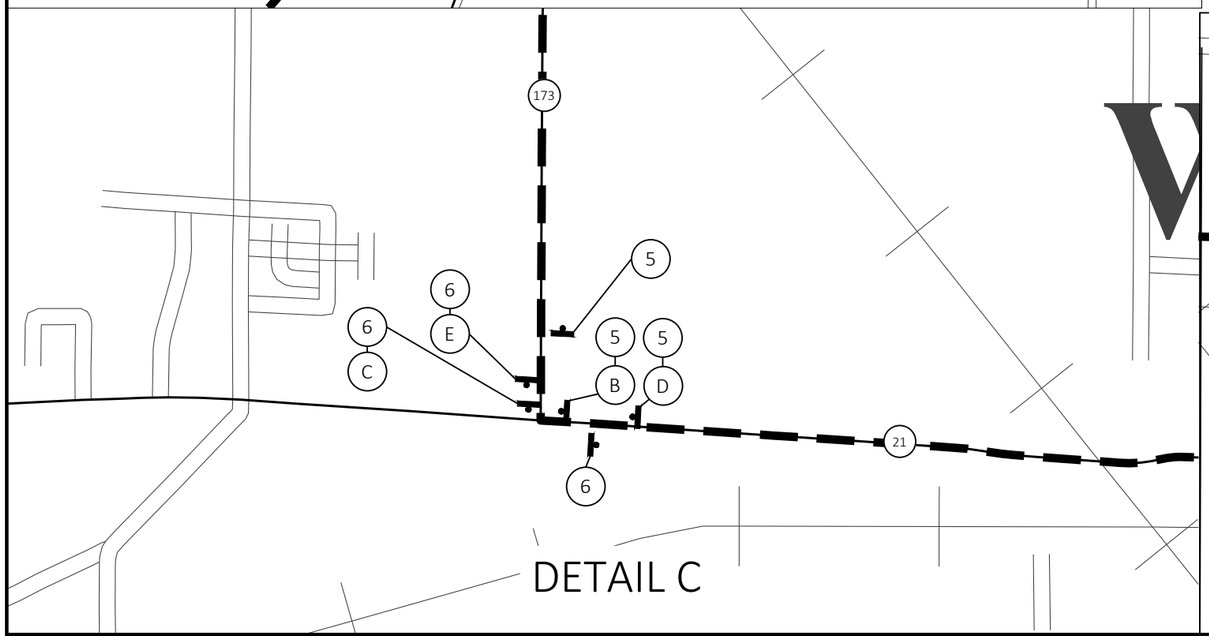
2



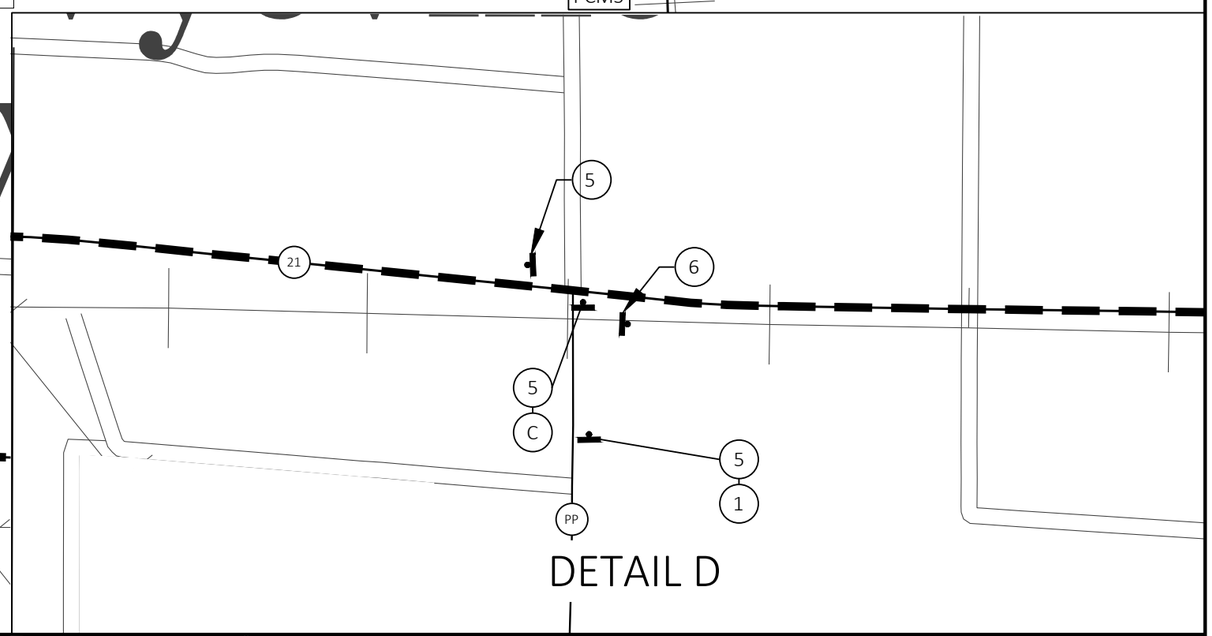
DETAIL A



DETAIL B



DETAIL C



DETAIL D

PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	DETOUR PLAN	SHEET 34	E

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\027001_DT.DWG
LAYOUT NAME - 2

PLOT DATE : 5/31/2022 9:22 AM PLOT BY : MARZOLF, ROBBI PLOT NAME : PLOT SCALE : ######

WISDOT/CADDS SHEET 42

LEGEND

- POST WITH ATTACHED SIGN
- EXISTING SIGN
- DETOUR ROUTE
- TYPE III BARRICADE WITH/WITHOUT SIGN
- COVER EXISTING SIGN

1 DETOUR AHEAD W20-2A

2 ROAD CLOSED AHEAD W20-3A

3 ROAD CLOSED 1000 FT W20-3C

4 ROAD CLOSED 500 FT W20-3D

5 DETOUR M4-8 24"X12" / NORTH M3-1 24"X12" / 80 M1-6 24"X24"

6 DETOUR M4-8 24"X12" / SOUTH M3-3 24"X12" / 80 M1-6 24"X24"

7 R11-3 60" X 30" ROAD CLOSED TO THRU TRAFFIC 3 MILES AHEAD — TYPE III BARRICADE

8 R11-3 60" X 30" ROAD CLOSED TO THRU TRAFFIC 1 MILES AHEAD — TYPE III BARRICADE

9 R11-2 48" X 30" ROAD CLOSED — TYPE III BARRICADE

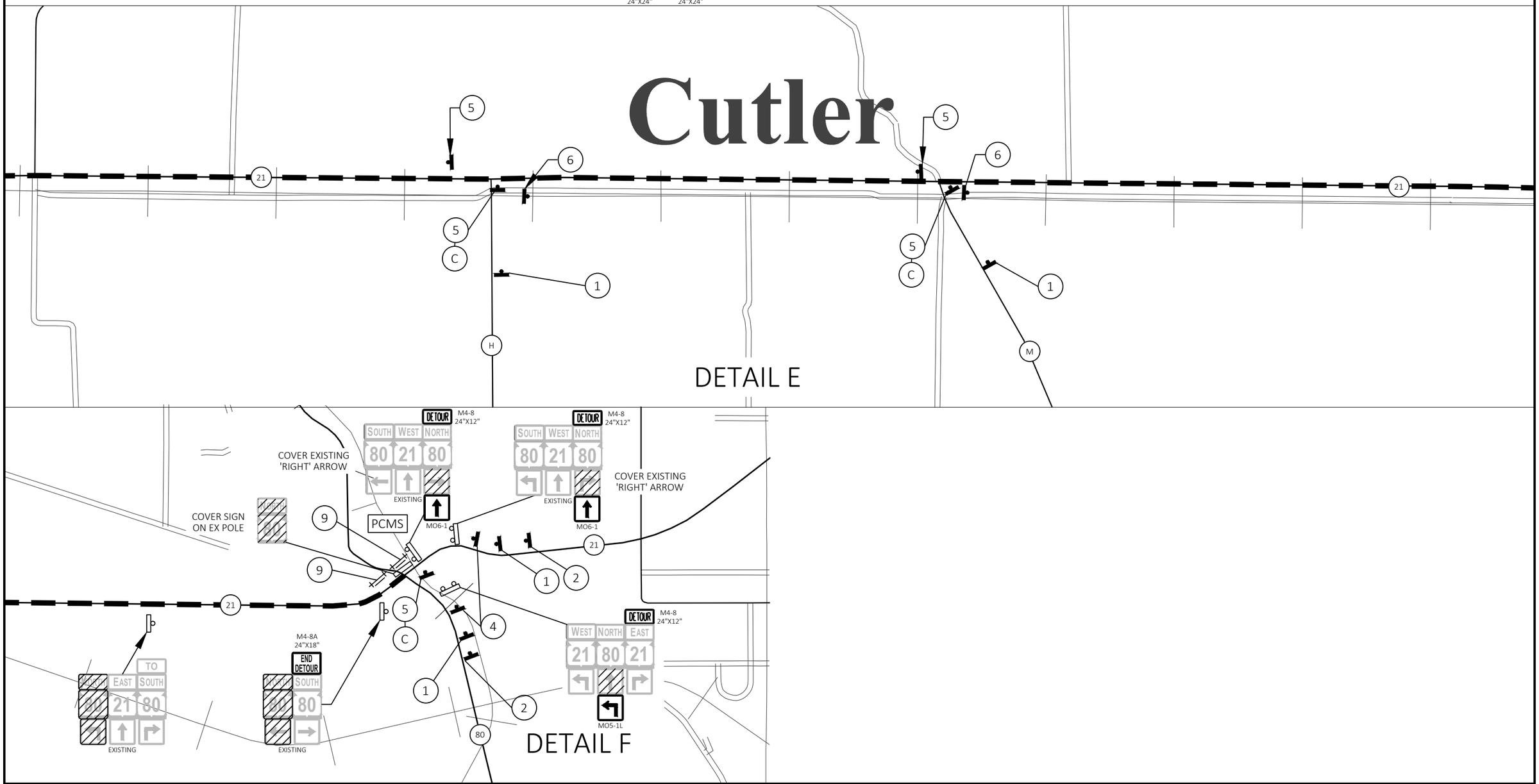
PCMS BOARDS

"HWY 80 CLOSED STARTING XX/XX/XXXX"

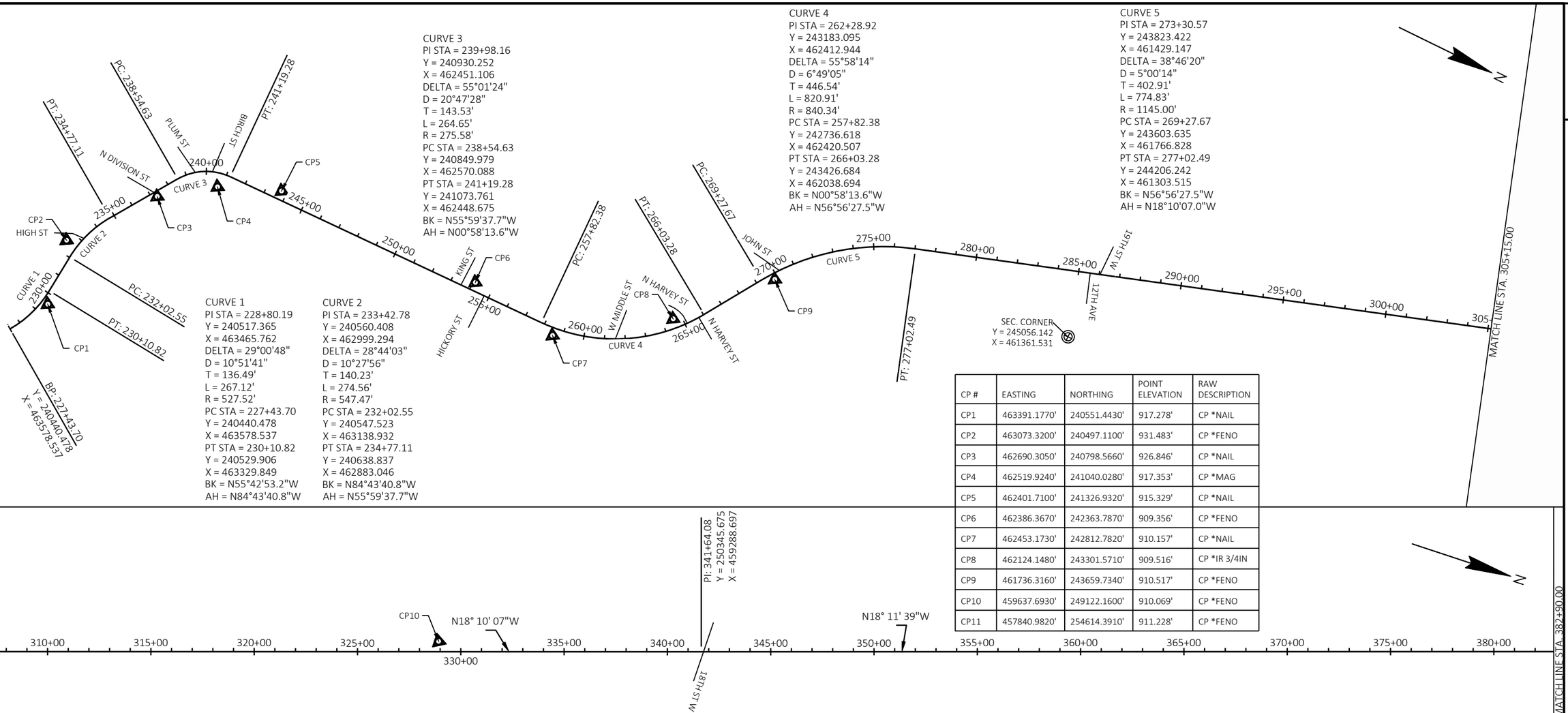
A MO6-1 | B MO6-1 | C MO6-1 | D MO5-1R | E MO5-1L | F MO6-2

N

Cutler



PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	DETOUR PLAN	SHEET 35	E



CURVE 1
PI STA = 228+80.19
Y = 240517.365
X = 463465.762
DELTA = 29°00'48"
D = 10°51'41"
T = 136.49'
L = 267.12'
R = 527.52'
PC STA = 227+43.70
Y = 240440.478
X = 463578.537
PT STA = 230+10.82
Y = 240529.906
X = 463329.849
BK = N55°42'53.2"W
AH = N84°43'40.8"W

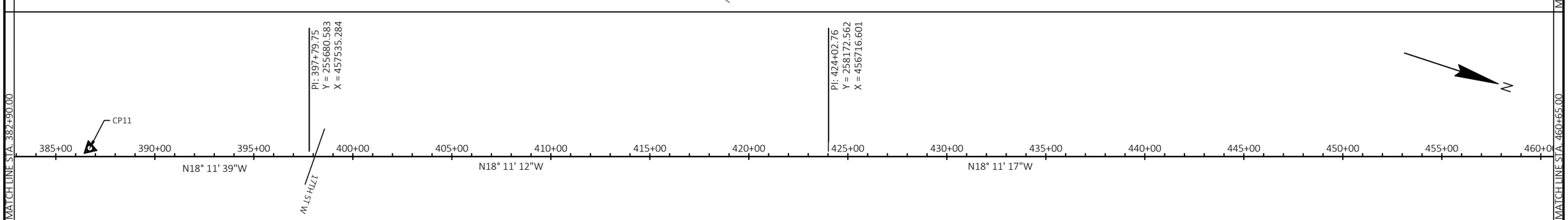
CURVE 2
PI STA = 233+42.78
Y = 240560.408
X = 462999.294
DELTA = 28°44'03"
D = 10°27'56"
T = 140.23'
L = 274.56'
R = 547.47'
PC STA = 232+02.55
Y = 240547.523
X = 463138.932
PT STA = 234+77.11
Y = 240638.837
X = 462883.046
BK = N84°43'40.8"W
AH = N55°59'37.7"W

CURVE 3
PI STA = 239+98.16
Y = 240930.252
X = 462451.106
DELTA = 55°01'24"
D = 20°47'28"
T = 143.53'
L = 264.65'
R = 275.58'
PC STA = 238+54.63
Y = 240849.979
X = 462570.088
PT STA = 241+19.28
Y = 241073.761
X = 462448.675
BK = N55°59'37.7"W
AH = N00°58'13.6"W

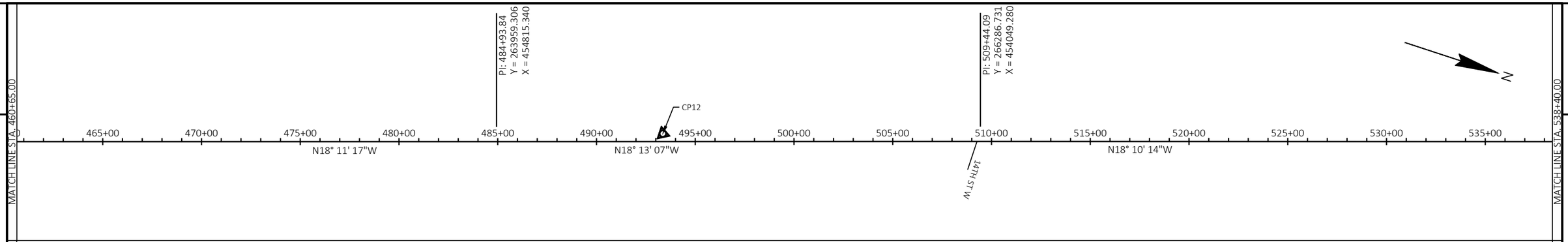
CURVE 4
PI STA = 262+28.92
Y = 243183.095
X = 462412.944
DELTA = 55°58'14"
D = 6°49'05"
T = 446.54'
L = 820.91'
R = 840.34'
PC STA = 257+82.38
Y = 242736.618
X = 462420.507
PT STA = 266+03.28
Y = 243426.684
X = 462038.694
BK = N00°58'13.6"W
AH = N56°56'27.5"W

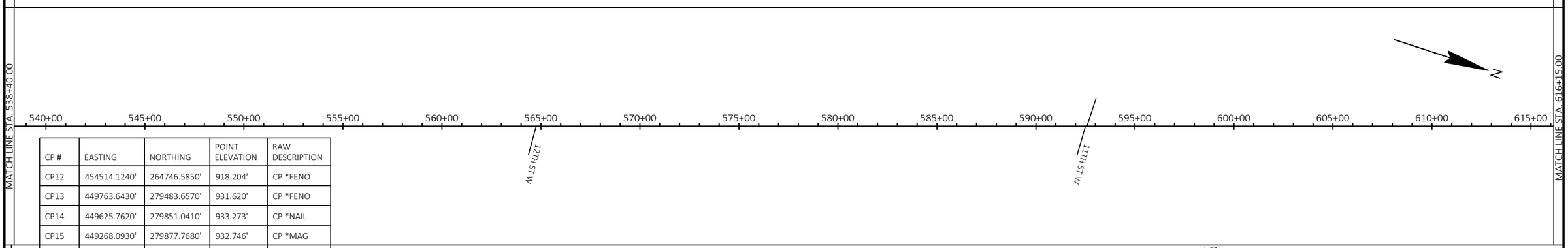
CURVE 5
PI STA = 273+30.57
Y = 243823.422
X = 461429.147
DELTA = 38°46'20"
D = 5°00'14"
T = 402.91'
L = 774.83'
R = 1145.00'
PC STA = 269+27.67
Y = 243603.635
X = 461766.828
PT STA = 277+02.49
Y = 244206.242
X = 461303.515
BK = N56°56'27.5"W
AH = N18°10'07.0"W

CP #	EASTING	NORTHING	POINT ELEVATION	RAW DESCRIPTION
CP1	463391.1770'	240551.4430'	917.278'	CP *NAIL
CP2	463073.3200'	240497.1100'	931.483'	CP *FENO
CP3	462690.3050'	240798.5660'	926.846'	CP *NAIL
CP4	462519.9240'	241040.0280'	917.353'	CP *MAG
CP5	462401.7100'	241326.9320'	915.329'	CP *NAIL
CP6	462386.3670'	242363.7870'	909.356'	CP *FENO
CP7	462453.1730'	242812.7820'	910.157'	CP *NAIL
CP8	462124.1480'	243301.5710'	909.516'	CP *IR 3/4IN
CP9	461736.3160'	243659.7340'	910.517'	CP *FENO
CP10	459637.6930'	249122.1600'	910.069'	CP *FENO
CP11	457840.9820'	254614.3910'	911.228'	CP *FENO

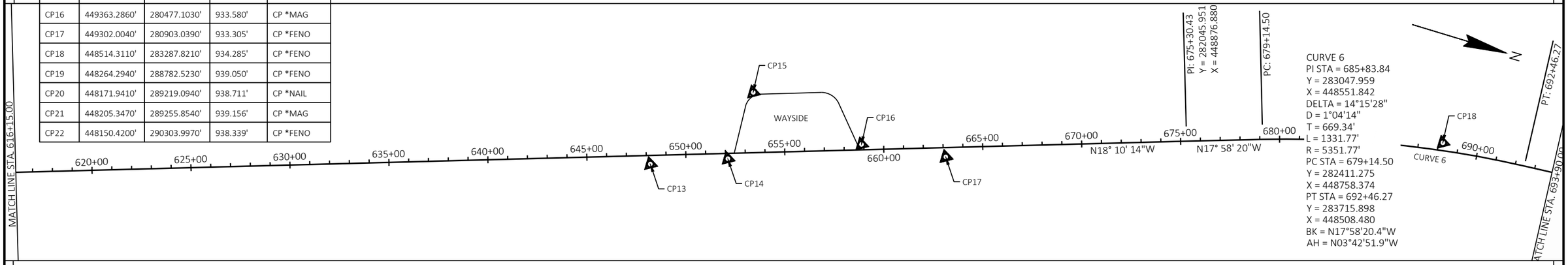


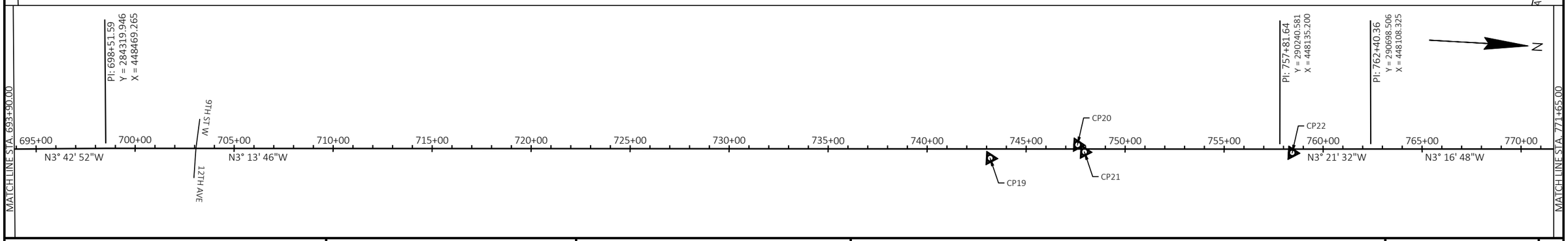
PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	ALIGNMENT DETAIL	SHEET 36	E





CP #	EASTING	NORTHING	POINT ELEVATION	RAW DESCRIPTION
CP12	454514.1240'	264746.5850'	918.204'	CP *FENO
CP13	449763.6430'	279483.6570'	931.620'	CP *FENO
CP14	449625.7620'	279851.0410'	933.273'	CP *NAIL
CP15	449268.0930'	279877.7680'	932.746'	CP *MAG
CP16	449363.2860'	280477.1030'	933.580'	CP *MAG
CP17	449302.0040'	280903.0390'	933.305'	CP *FENO
CP18	448514.3110'	283287.8210'	934.285'	CP *FENO
CP19	448264.2940'	288782.5230'	939.050'	CP *FENO
CP20	448171.9410'	289219.0940'	938.711'	CP *NAIL
CP21	448205.3470'	289255.8540'	939.156'	CP *MAG
CP22	448150.4200'	290303.9970'	938.339'	CP *FENO



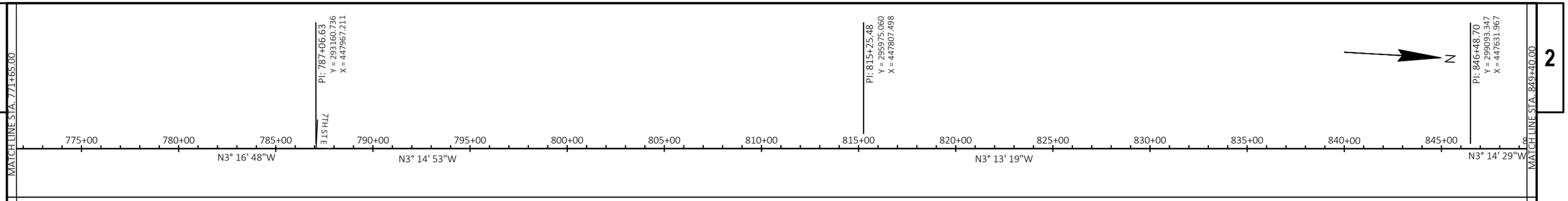


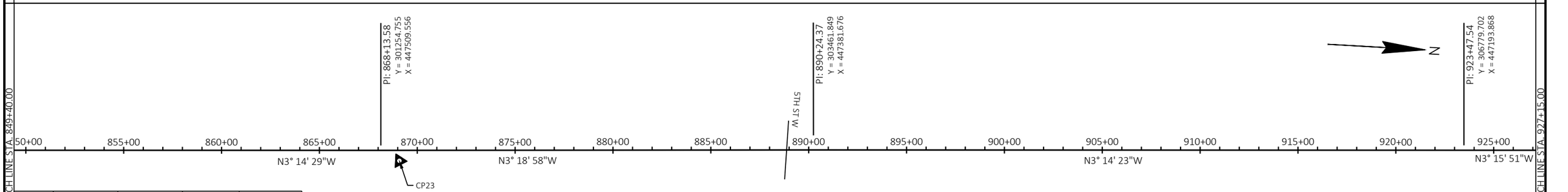
PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	ALIGNMENT DETAIL	SHEET 37	E

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\027201_AD.DWG
LAYOUT NAME - 2

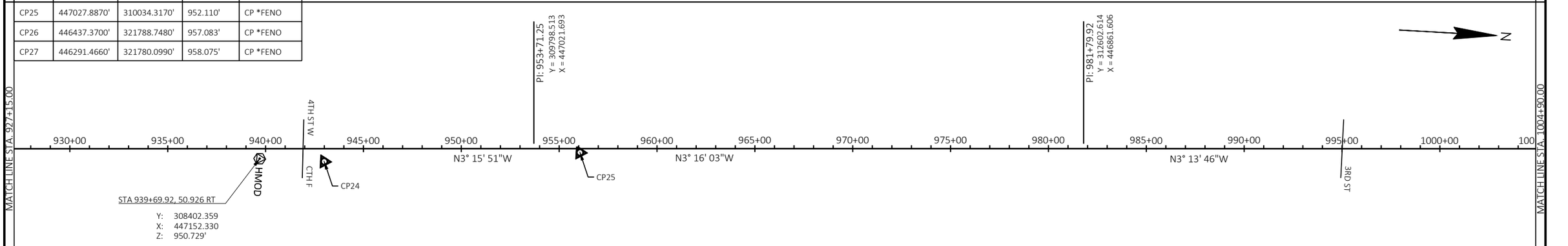
PLOT DATE : 5/31/2022 9:25 AM PLOT BY : MARZOLF, ROBBI PLOT NAME : PLOT SCALE : 1 IN:500 FT

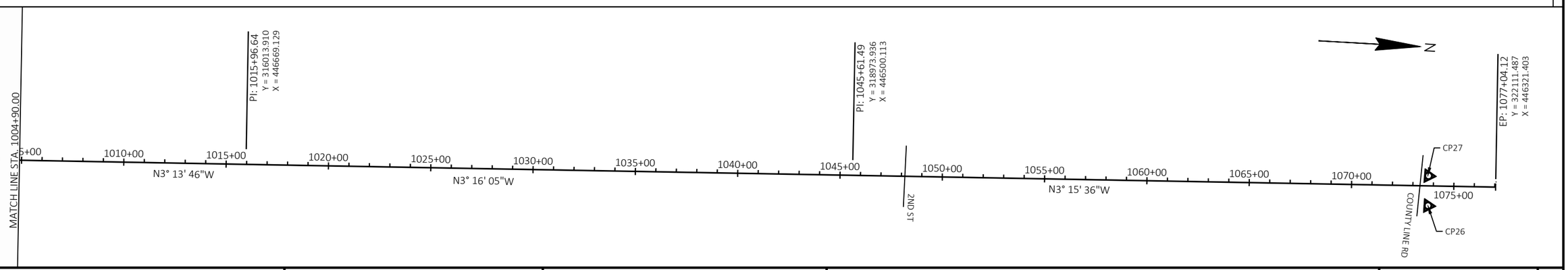
WISDOT/CADDS SHEET 44





CP #	EASTING	NORTHING	POINT ELEVATION	RAW DESCRIPTION
CP23	447557.2570'	301352.4890'	943.279'	CP *FENO
CP24	447148.3240'	308732.2490'	948.700'	CP *FENO
CP25	447027.8870'	310034.3170'	952.110'	CP *FENO
CP26	446437.3700'	321788.7480'	957.083'	CP *FENO
CP27	446291.4660'	321780.0990'	958.075'	CP *FENO





PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	ALIGNMENT DETAIL	SHEET 38	E

Estimate Of Quantities

1620-02-78

Line	Item	Item Description	Unit	Total	Qty
0002	203.0100	Removing Small Pipe Culverts	EACH	9.000	9.000
0004	204.0110	Removing Asphaltic Surface	SY	407.000	407.000
0006	204.0115	Removing Asphaltic Surface Butt Joints	SY	1,269.000	1,269.000
0008	204.0120	Removing Asphaltic Surface Milling	SY	271,856.000	271,856.000
0010	204.0150	Removing Curb & Gutter	LF	1,178.000	1,178.000
0012	204.0155	Removing Concrete Sidewalk	SY	510.000	510.000
0014	204.9060.S	Removing (item description) 01. Removing Concrete Headwall	EACH	2.000	2.000
0016	204.9060.S	Removing (item description) 02. Removing Bollards	EACH	8.000	8.000
0018	205.0100	Excavation Common	CY	2,575.000	2,575.000
0020	211.0101	Prepare Foundation for Asphaltic Paving (project) 01. 1620-02-78	EACH	1.000	1.000
0022	213.0100	Finishing Roadway (project) 01. 1620-02-78	EACH	1.000	1.000
0024	305.0110	Base Aggregate Dense 3/4-Inch	TON	8,363.000	8,363.000
0026	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	250.000	250.000
0028	330.0100	Mill and Relay	SY	271,860.000	271,860.000
0030	374.1010.S	QMP Mill and Relay Compaction	SY	271,860.000	271,860.000
0032	455.0605	Tack Coat	GAL	33,513.000	33,513.000
0034	460.0105.S	HMA Percent Within Limits (PWL) Test Strip Volumetrics	EACH	2.000	2.000
0036	460.0110.S	HMA Percent Within Limits (PWL) Test Strip Density	EACH	2.000	2.000
0038	460.2000	Incentive Density HMA Pavement	DOL	1,300.000	1,300.000
0040	460.2005	Incentive Density PWL HMA Pavement	DOL	49,750.000	49,750.000
0042	460.2007	Incentive Density HMA Pavement Longitudinal Joints	DOL	33,718.000	33,718.000
0044	460.2010	Incentive Air Voids HMA Pavement	DOL	62,340.000	62,340.000
0046	460.6223	HMA Pavement 3 MT 58-28 S	TON	37,194.000	37,194.000
0048	460.6224	HMA Pavement 4 MT 58-28 S	TON	26,659.000	26,659.000
0050	460.6424	HMA Pavement 4 MT 58-28 H	TON	2,432.000	2,432.000
0052	465.0120	Asphaltic Surface Driveways and Field Entrances	TON	25.000	25.000
0054	465.0475	Asphalt Centerline Rumble Strips 2-Lane Rural	LF	75,146.000	75,146.000
0056	522.0124	Culvert Pipe Reinforced Concrete Class III 24-Inch	LF	145.000	145.000
0058	522.1024	Apron Endwalls for Culvert Pipe Reinforced Concrete 24-Inch	EACH	6.000	6.000
0060	522.2343	Culvert Pipe Reinforced Concrete Horizontal Elliptical Class HE-III 43x68-Inch	LF	192.000	192.000
0062	522.2643	Apron Endwalls for Culvert Pipe Reinforced Concrete Horizontal Elliptical 43x68-Inch	EACH	6.000	6.000
0064	601.0411	Concrete Curb & Gutter 30-Inch Type D	LF	1,178.000	1,178.000
0066	601.0600	Concrete Curb Pedestrian	LF	13.000	13.000
0068	602.0405	Concrete Sidewalk 4-Inch	SF	5,206.000	5,206.000
0070	602.0505	Curb Ramp Detectable Warning Field Yellow	SF	280.000	280.000
0072	602.0605	Curb Ramp Detectable Warning Field Radial Yellow	SF	130.000	130.000
0074	611.0430	Reconstructing Inlets	EACH	25.000	25.000
0076	611.0624	Inlet Covers Type H	EACH	25.000	25.000
0078	611.8110	Adjusting Manhole Covers	EACH	8.000	8.000
0080	618.0100	Maintenance And Repair of Haul Roads (project) 01. 1620-02-78	EACH	1.000	1.000
0082	619.1000	Mobilization	EACH	1.000	1.000
0084	624.0100	Water	MGAL	1,214.000	1,214.000
0086	625.0100	Topsoil	SY	3,840.000	3,840.000
0088	625.0500	Salvaged Topsoil	SY	7,840.000	7,840.000
0090	627.0200	Mulching	SY	3,840.000	3,840.000
0092	628.1504	Silt Fence	LF	5,772.000	5,772.000
0094	628.1520	Silt Fence Maintenance	LF	5,772.000	5,772.000
0096	628.1905	Mobilizations Erosion Control	EACH	4.000	4.000
0098	628.1910	Mobilizations Emergency Erosion Control	EACH	3.000	3.000

1620-02-78

Line	Item	Item Description	Unit	Total	Qty
0100	628.2006	Erosion Mat Urban Class I Type A	SY	7,840.000	7,840.000
0102	628.7015	Inlet Protection Type C	EACH	33.000	33.000
0104	628.7504	Temporary Ditch Checks	LF	231.000	231.000
0106	628.7555	Culvert Pipe Checks	EACH	54.000	54.000
0108	629.0210	Fertilizer Type B	CWT	7.400	7.400
0110	630.0140	Seeding Mixture No. 40	LB	210.900	210.900
0112	630.0500	Seed Water	MGAL	91.000	91.000
0114	633.5200	Markers Culvert End	EACH	12.000	12.000
0116	634.0616	Posts Wood 4x6-Inch X 16-FT	EACH	12.000	12.000
0118	638.2102	Moving Signs Type II	EACH	12.000	12.000
0120	638.3000	Removing Small Sign Supports	EACH	12.000	12.000
0122	642.5201	Field Office Type C	EACH	1.000	1.000
0124	643.0420	Traffic Control Barricades Type III	DAY	860.000	860.000
0126	643.0705	Traffic Control Warning Lights Type A	DAY	1,640.000	1,640.000
0128	643.0900	Traffic Control Signs	DAY	2,903.000	2,903.000
0130	643.0920	Traffic Control Covering Signs Type II	EACH	9.000	9.000
0132	643.1050	Traffic Control Signs PCMS	DAY	14.000	14.000
0134	643.5000	Traffic Control	EACH	1.000	1.000
0136	644.1430	Temporary Pedestrian Surface Plate	SF	2,577.000	2,577.000
0138	644.1601	Temporary Pedestrian Curb Ramp	DAY	1,029.000	1,029.000
0140	644.1810	Temporary Pedestrian Barricade	LF	300.000	300.000
0142	646.1020	Marking Line Epoxy 4-Inch	LF	210,933.000	210,933.000
0144	646.6120	Marking Stop Line Epoxy 18-Inch	LF	241.000	241.000
0146	646.7420	Marking Crosswalk Epoxy Transverse Line 6-Inch	LF	1,334.000	1,334.000
0148	648.0100	Locating No-Passing Zones	MI	15.200	15.200
0150	649.0105	Temporary Marking Line Paint 4-Inch	LF	78,739.000	78,739.000
0152	649.0120	Temporary Marking Line Epoxy 4-Inch	LF	33,010.000	33,010.000
0154	649.0150	Temporary Marking Line Removable Tape 4-Inch	LF	460.000	460.000
0156	650.5500	Construction Staking Curb Gutter and Curb & Gutter	LF	1,178.000	1,178.000
0158	650.6000	Construction Staking Pipe Culverts	EACH	6.000	6.000
0160	650.8000	Construction Staking Resurfacing Reference	LF	84,296.000	84,296.000
0162	650.9000	Construction Staking Curb Ramps	EACH	28.000	28.000
0164	650.9911	Construction Staking Supplemental Control (project) 01. 1620-02-78	EACH	1.000	1.000
0166	690.0150	Sawing Asphalt	LF	2,773.000	2,773.000
0168	690.0250	Sawing Concrete	LF	140.000	140.000
0170	740.0440	Incentive IRI Ride	DOL	63,861.000	63,861.000
0172	ASP.1T0A	On-the-Job Training Apprentice at $5.00/HR	HRS	2,500.000	2,500.000
0174	ASP.1T0G	On-the-Job Training Graduate at $5.00/HR	HRS	3,520.000	3,520.000
0176	SPV.0060	Special 01. Adjusting Sanitary Manhole Covers	EACH	4.000	4.000
0178	SPV.0060	Special 02. Adjusting Water Valves	EACH	9.000	9.000
0180	SPV.0060	Special 03. Temporary Water Diversion B-29-73	EACH	1.000	1.000
0182	SPV.0195	Special 01. Salvaged Asphaltic Pavement Milling	TON	1,096.000	1,096.000
0184	SPV.0195	Special 02. Salvaged Asphaltic Pavement Base	TON	1,096.000	1,096.000

3

REMOVING SMALL PIPE CULVERTS

CATEGORY	STATION	LOCATION	203.0100 EACH	REMARKS
0010	327+56	STH 80	1	24” X 45’ CORRUGATED STEEL PIPE
0010	500+38	STH 80	1	24” X 50’ CORRUGATED STEEL PIPE
0010	652+45	LT	1	WAYSIDE: 1 - 18"X70' CMCP
0010	658+44	LT	1	WAYSIDE: 1 - 18"X70' CMCP
0010	1068+17	STH 80	1	24” X 50’ CORRUGATED STEEL PIPE
	CAT 0010 SUBTOTAL=		**5**	
0020	747+59	CL	4	B-29-73: 4 - 64"x43"x68' STEEL PLATE PIPE ARCHES
	CAT 0020 SUBTOTAL=		4	
	PROJECT TOTAL =		**9**	

REMOVING CONCRETE HEADWALLS

CATEGORY	STATION	LOCATION	204.9060.S.01 EACH	REMARKS
0020	747+59	LT & RT	2	B-29-73
	CAT 0020 SUBTOTAL=		**2**	

3

REMOVING BOLLARDS

CATEGORY	STATION	TO	STATION	LOCATION	204.9060.S.02 EACH	REMARKS
0010	652+28	-	658+62	LT	8	WAYSIDE
			CAT 0010 SUBTOTAL=		**8**	

PAVEMENT REMOVAL SUMMARY

CATEGORY	STATION	TO	STATION	LOCATION	REMOVING ASPHALTIC SURFACE 204.0110 SY	REMOVING ASPHALTIC SURFACE BUTT JOINTS 204.0115 SY	REMOVING ASPHALTIC SURFACE MILLING 204.0120 SY	REMARKS
0010	230+32	-	230+82	BEGIN PROJECT		180		
0010	230+82	-	271+12	MAINLINE URBAN	-	-	17,036	
0010	232+81	-	269+96	URBAN SIDEROADS	-	445	-	
0010	271+12	-	271+62	END URBAN SECTION	-	194	-	
0010	271+62	-	1072+63	MAINLINE RURAL	-	-	271,856	***
0010	746+98	-	748+21	STRUCTURE REMOVAL	407	-	-	
0010	1072+63	-	1073+13	END PROJECT	-	175	-	
0010	285+48	-	1048+28	SIDEROADS	-	455	-	
0010		-		DRIVEWAYS	-	-	-	
				PROJECT TOTALS =	**407**	**1,269**	**271,856**	

*** MILL AND REMOVE TOP 2-INCHES, THEN MILL AND RELAY THE REMAINING ASPHALTIC PAVEMENT STRUCTURE.

PREPARE FOUNDATION FOR ASPHALTIC PAVING

CATEGORY	STATION	TO	STATION	LOCATION	211.0101 EACH
0010	230+32	-	1073+28	STH 80	1
				PROJECT TOTAL =	**1**

3

SIDEWALK SUMMARY

CATEGORY	STATION	TO	STATION	LOCATION	REMOVING CONCRETE SIDEWALK 204.0155 SY	CONCRETE CURB PEDESTRIAN 601.0600 LF	CONCRETE SIDEWALK 4-INCH 602.0405 SF	CURB RAMP DETECTABLE WARNING FIELD YELLOW 602.0505 SF	CURB RAMP DETECTABLE WARNING FIELD RADIAL YELLOW 602.0605 SF
0010	232+86	-	232+86	HIGH ST. SE	20	-	180	10	-
0010	233+32	-	233+32	HIGH ST. SW	13	-	126	10	-
0010	236+76	-	236+76	N. DIVISION ST. SE	28	-	152	10	-
0010	237+25	-	237+25	N. DIVISION ST. SW	18	-	177	10	-
0010	239+16	-	239+16	PLUM ST. SE	15	-	149	10	-
0010	239+50	-	239+50	PLUM ST. SW	37	-	460	10	27
0010	239+54	-	239+54	PLUM ST. NW	17	-	198	20	-
0010	240+20	-	240+20	BIRCH ST. SW	13	-	126	10	-
0010	240+74	-	240+74	BIRCH ST. NW	22	-	215	-	23
0010	243+89	-	243+89	MAPLE ST. SW	20	-	232	20	-
0010	243+92	-	243+92	MAPLE ST. SE	22	-	206	10	-
0010	244+34	-	244+34	MAPLE ST. NW	26	-	251	10	-
0010	253+16	-	253+16	KING ST. SW	19	-	172	-	14
0010	253+94	-	253+94	KING ST. NW	19	-	146	10	14
0010	254+29	-	254+53	HICKORY ST. SE	41	13	311	20	-
0010	254+92	-	254+92	HICKORY ST. NE	14	-	143	10	-
0010	261+27	-	261+27	W. MIDDLE ST. SW	13	-	218	20	-
0010	261+36	-	261+36	W. MIDDLE ST. SE	18	-	179	10	-
0010	262+16	-	262+16	W. MIDDLE ST. NW	17	-	158	-	18
0010	264+62	-	264+86	N. HARVEY ST. SE	14	-	178	20	-
0010	264+54	-	264+54	N. HARVEY MID BLOCK	20	-	247	10	-
0010	265+30	-	265+63	N. HARVEY ST. SW	23	-	167	10	-
0010	265+05	-	265+36	N. HARVEY ST. NE	17	-	195	-	19
0010	266+03	-	266+21	N. HARVEY ST. NW	-	-	90	10	-
0010	266+60	-	266+60	NORTH ST. SE	14	-	145	10	-
0010	267+02	-	267+02	NORTH ST. SW	13	-	129	10	-
0010	269+60	-	269+60	JOHN ST. SE	17	-	158	-	15
				JOHN ST. SW	-		98	10	-
				PROJECT TOTALS =	**510**	**13**	**5,206**	**280**	**130**

CURB & GUTTER SUMMARY

CATEGORY	STATION	TO	STATION	LOCATION	REMOVING CURB & GUTTER 204.0150 LF	CONCRETE CURB & GUTTER 30-INCH TYPE D 601.0411 LF
0010	232+86	-	232+86	HIGH ST SW	23	23
0010	233+32	-	233+32	HIGH ST NW	23	23
0010	236+76	-	236+76	DIVISION ST SW	25	25
0010	237+25	-	237+25	DIVISION ST NW	24	24
0010	239+16	-	239+16	PLUM ST SW	21	21
0010	239+50	-	239+50	PLUM ST NW	48	48
0010	239+54	-	239+54	PLUM ST NE	26	26
0010	240+20	-	240+20	BIRCH ST SW	22	22
0010	240+74	-	240+74	BIRCH ST NW	33	33
0010	243+89	-	243+89	MAPLE ST SW	47	47
0010	243+92	-	243+92	MAPLE ST SE	38	38
0010	244+34	-	244+34	MAPLE ST NW	30	30
0010	253+16	-	253+16	KING ST SW	26	26
0010	253+94	-	253+94	KING ST NW	57	57
0010	254+52	-	254+52	HICKORY ST SE	56	56
0010	254+92	-	254+92	HICKORY ST NE	22	22
0010	261+27	-	261+27	MIDDLE ST SW	38	38
0010	261+36	-	261+36	MIDDLE ST SE	22	22
0010	262+16	-	262+16	MIDDLE ST NW	25	25
0010	264+85	-	264+85	HARVEY ST SW	37	37
0010	264+86	-	264+86	HARVEY ST EAST MID	21	21
0010	265+32	-	265+32	HARVEY ST NW	52	52
0010	265+32	-	265+31	HARVEY ST SE	30	30
0010	265+32	-	265+32	HARVEY ST NE	25	25
0010	266+60	-	266+60	NORTH ST SW	21	21
0010	267+02	-	267+02	NORTH ST NW	22	22
0010	269+60	-	269+60	JOHN ST SW	24	24
0010	269+60	-	269+61	JOHN ST NW	26	26
0010	941+56	-	941+56	CTH F SE	80	80
0010	942+14	-	942+14	CTH F NE	69	69
0010	231+06	-	268+10	INLET REPAIR	165	165
				PROJECT TOTALS =	**1,178**	**1,178**

DIVISION	FROM/TO STATION	LOCATION	205.0100 COMMON EXCAVATION (1) CUT (2)	205.0100 COMMON EXCAVATION (1) EBS EXCAVATION (3)	SALVAGED/UNUSABLE PAVEMENT MATERIAL (4)	AVAILABLE MATERIAL (5)	205.0500 MARSH EXCAVATION	205.0200 ROCK EXCAVATION	REDUCED MARSH IN FILL FACTOR 0.60	REDUCED EBS IN FILL FACTOR 0.80	EXPANDED MARSH BACKFILL FACTOR 1.50	EXPANDED EBS BACKFILL FACTOR 1.30	EXPANDED ROCK FACTOR 1.10	UNEXPANDED FILL	EXPANDED FILL (13) FACTOR 1.25	MASS ORDINATE +/- (14)	WASTE	208.0100 BORROW	COMMENT
DIVISION 1																			
CURB REPLACE	230+32-270+00	STH 80	208	0	0	208	0	0	0	0	0	0	0	0	0	208	208	0	
DIVISION 1 SUBTOTAL			208	0	0	208	0	0	0	0	0	0	0	0	0	208	208	0	
DIVISION 2																			
WAYSIDE/BYPASS LANE/TURN LANE REMOVAL	648+50-663+00	STH 80	2,129	0	551	1,578	0	0	0	0	0	0	0	60	75	1,503	1,503	0	
DIVISION 2 SUBTOTAL			2,129	0	551	1,578	0	0	0	0	0	0	0	60	75	1,503	1,503	0	
DIVISION 3																			
B-27-73 STRUCTURE REMOVAL	746+98-748+21	STH 80	238	0	48	190	0	0	0	0	0	0	0	100	125	65	65	0	
DIVISION 3 SUBTOTAL			238	0	551	190	0	0	0	0	0	0	0	100	125	65	65	0	
GRAND TOTAL			2,575	0	1,102	1,976	0	0	0	0	0	0	0	160	200	1,776	1,776	0	
TOTAL COMMON EXC			2,575																

NOTES:

(1) COMMON EXCAVATION IS THE SUM OF THE CUT AND EBS EXCAVATION COLUMNS. ITEM NUMBER 205.0100

(2) SALVAGED/UNSUABLE PAVEMENT MATERIAL IS INCLUDED IN CUT.

(3) EBS EXCAVATION TO BE BACKFILLED WITH SELECT BORROW MATERIAL. NOTE: THIS IS DESIGNERS CHOICE, CAN BE BACKFILLED WITH BORROW, OR CUT AS WELL.

(4) SALVAGED/UNUSABLE PAVEMENT MATERIAL

(5) AVAILABLE MATERIAL = CUT - SALVAGED/UNUSUABLE PAVEMENT MATERIAL

(13) EXPANDED FILL FACTOR = 1.25

DEPENDING ON SELECTIONS: **EXPANDED FILL = (UNEXPANDED FILL - EXPANDED ROCK - REDUCED MARSH - REDUCED EBS) * FILL FACTOR**

OR EXPANDED FILL = (UNEXPANDED FILL - EXPANDED ROCK - REDUCED EBS) * FILL FACTOR

OR EXPANDED FILL = (UNEXPANDED FILL - EXPANDED ROCK - REDUCED MARSH) * FILL FACTOR

OR EXPANDED FILL = (UNEXPANDED FILL - EXPANDED ROCK) * FILL FACTOR

(14) THE MASS ORDINATE + OR - QTY CALCULATED FOR THE DIVISION. PLUS QUANTITY INDICATES AN EXCESS OF MATERIAL WITHIN THE DIVISION. MINUS INDICATES A SHORTAGE OF MATERIAL WITHIN THE DIVISION.

(15) FACTORS USED TO COMPUTE ANTICIPATED WASTE AND THE COMPUTED WASTE VOLUME IDENTIFIED ARE FOR GENERAL INFORMATION ONLY.

BASE AGGREGATE DENSE 3/4-INCH

CATEGORY	STATION	TO	STATION	LOCATION	LOCATION	305.0110 TON	REMARKS
0010	271+12	-	285+80	LT	C&G TO 19TH ST	79	AGG. SHOULDER
0010	271+12	-	285+80	RT	C&G TO 12TH AVE	76	AGG. SHOULDER
0010	285+80	-	341+80	LT	19TH ST TO 18TH ST	287	AGG. SHOULDER
0010	285+80	-	341+80	RT	12TH AVE TO 18TH ST	293	AGG. SHOULDER
0010	341+80	-	398+08	LT	18TH ST TO 17TH ST	291	AGG. SHOULDER
0010	341+80	-	398+08	RT	18TH ST TO 17TH ST	295	AGG. SHOULDER
0010	398+08	-	509+12	RT	17TH ST TO 14TH ST	574	AGG. SHOULDER
0010	398+08	-	592+48	LT	17TH ST TO 11TH ST	1,000	AGG. SHOULDER
0010	509+12	-	564+62	RT	14TH ST TO 12TH ST	289	AGG. SHOULDER
0010	564+62	-	592+48	RT	12TH ST TO 11TH ST	148	AGG. SHOULDER
0010	592+48	-	703+04	LT	11TH ST TO 12TH AVE	567	AGG. SHOULDER
0010	592+48	-	703+04	RT	11TH ST TO 12TH AVE	574	AGG. SHOULDER
0010	703+04	-	787+09	LT	12TH AVE TO 7TH ST	430	AGG. SHOULDER
0010	703+04	-	888+88	RT	12TH AVE TO 5TH ST	961	AGG. SHOULDER
0010	787+09	-	888+88	LT	7TH ST TO 5TH ST	523	AGG. SHOULDER
0010	888+88	-	941+92	LT	5TH ST TO 4TH ST	274	AGG. SHOULDER
0010	888+88	-	941+92	RT	5TH ST TO CTH F	273	AGG. SHOULDER
0010	941+92	-	995+00	LT	4TH ST TO 3RD ST	275	AGG. SHOULDER
0010	941+92	-	995+00	RT	CTH F TO 3RD ST	271	AGG. SHOULDER
0010	995+00	-	1048+16	LT	3RD ST TO 2ND ST	274	AGG. SHOULDER
0010	995+00	-	1048+16	RT	3RD ST TO 2ND ST	275	AGG. SHOULDER
0010	1048+16	-	1073+24	LT	2ND ST TO COUNTY LINE RD	129	AGG. SHOULDER
0010	1048+16	-	1073+24	RT	2ND ST TO COUNTY LINE RD	129	AGG. SHOULDER
0010	279+58	-	-	LT	STH 80	7	DRIVEWAY
0010	290+08	-	-	LT	STH 80	3	DRIVEWAY
0010	313+88	-	-	LT	STH 80	5	DRIVEWAY
0010	327+28	-	-	LT	STH 80	5	DRIVEWAY
0010	493+28	-	-	LT	STH 80	3	DRIVEWAY
0010	546+80	-	-	LT	STH 80	2	DRIVEWAY
0010	626+54	-	-	LT	STH 80	9	DRIVEWAY
0010	672+84	-	-	LT	STH 80	3	DRIVEWAY
0010	685+24	-	-	LT	STH 80	3	DRIVEWAY
0010	706+28	-	-	LT	STH 80	6	DRIVEWAY
0010	973+04	-	-	LT	STH 80	4	DRIVEWAY
0010	974+84	-	-	LT	STH 80	5	DRIVEWAY
0010	975+04	-	-	RT	STH 80	2	DRIVEWAY
0010	1015+40	-	-	LT	STH 80	4	DRIVEWAY
0010	1034+94	-	-	LT	STH 80	7	DRIVEWAY
0010	1061+84	-	-	LT	STH 80	8	DRIVEWAY
					PROJECT TOTAL =	**8,363**	

BASE AGGREGATE DENSE 1 1/4-INCH

CATEGORY	STATION	TO	STATION	LOCATION	305.0120 TON	REMARKS
0010	UNDISTRIBUTED				250	CURB & GUTTER REPLACEMENT SECTIONS
				PROJECT TOTAL =	**250**	

MILL AND RELAY SUMMARY

CATEGORY	STATION	TO	STATION	LOCATION	MILL AND RELAY 330.0100 SY	QMP MILL AND RELAY COMPACTION 374.1020.S SY	REMARKS
0010	271+12	-	1073+28	MAINLINE RURAL	271,860	271,860	
				PROJECT TOTALS =	**271,860**	**271,860**	

SALVAGED ASPHALTIC PAVEMENT SUMMARY

CATEGORY	STATION	TO	STATION	LOCATION	SALVAGED ASPHALTIC PAVEMENT MILLING SPV.0195.01 TON	SALVAGED ASPHALTIC PAVEMENT BASE SPV.0195.02 TON	REMARKS
0010	327+04	-	328+08	STH 80	262	262	CULVERT REPLACEMENT
0010	499+86	-	500+90	STH 80	262	262	CULVERT REPLACEMENT
0010	746+98	-	748+21	STH 80	310	310	CULVERT REPLACEMENT
0010	1067+65	-	1068+69	STH 80	262	262	CULVERT REPLACEMENT
				PROJECT TOTALS=	**1,096**	**1,096**	

3

ASPHALT ITEMS SUMMARY

CATEGORY	STATION	TO	STATION	LOCATION	HMA PAVEMENT 3 MT 58-28 S 460.6223 TON	HMA PAVEMENT 4 MT 58-28 S 460.6224 TON	HMA PAVEMENT 4 MT 58-28 H 460.6424 TON	TACK COAT 455.0605 GAL	ASPHALTIC SURFACE DRIVEWAY AND FIELD ENTRANCES 465.0120 TON	REMARKS
0010	230+32	-	271+12	MAINLINE URBAN	2,918	-	-	-	-	BINDER LAYER
0010	230+32	-	271+12	MAINLINE URBAN	-	-	2,432	869	-	SURFACE LAYER
0010	271+12	-	1073+28	MAINLINE RURAL	34,276	-	-	19,042	-	BINDER LAYER
0010	271+12	-	1073+28	MAINLINE RURAL	-	26,659	-	13,602	-	SURFACE LAYER
0010	230+32	-	1073+28	UNDISTRIBUTED	-	-	-	-	25	
				PROJECT TOTALS =	**37,194**	**26,659**	**2,432**	**33,513**	**25**	

3

HMA PAVEMENT PWL TEST STRIPS

CATEGORY	STATION	TO	STATION	LOCATION	HMA PERCENT WITHIN LIMITS (PWL) TEST STRIP VOLUMETRICS 460.0105.S EACH	HMA PERCENT WITHIN LIMITS (PWL) TEST STRIP DENSITY 460.0110.S EACH
0010	230+32	-	1073+28	STH 80	2	2
				PROJECT TOTALS =	2	2

FOR INFORMATIONAL PURPOSES ONLY

STATIONS	LOCATION	MIXTURE USE	UNDERLYING SURFACE	BID ITEM	TONS	THICKNESS (IN)	MIXTURE ACCEPTANCE	DENSITY ACCEPTANCE
230+32-271+12	2X12 FT TRAVEL LANE	LOWER LAYER	MILLED EXISTING HMA SURFACE	3 MT 58-28 S	1,828	3	PWL INCENTIVE AIR VOIDS HMA PAVEMENT 460.2010	INCENTIVE DENSITY PWL HMA PAVEMENT 460.2005
230+32-271+12	2X12 FT TRAVEL LANE	UPPER LAYER	HMA PAVEMENT 3 MT 58-28 S	4 MT 58-28 H	1,523	2.5	QMP AS PER SS 460	INCENTIVE DENSITY HMA PAVEMENT 460.2000
230+32-271+12	2X4 FT SHOULDER	LOWER LAYER	MILLED EXISTING HMA SURFACE	3 MT 58-28 S	609	3	PWL INCENTIVE AIR VOIDS HMA PAVEMENT 460.2010	ACCEPTANCE TESTING BY THE DEPARTMENT; NOT ELIGIBLE FOR INCENTIVE
230+32-271+12	2X4 FT SHOULDER	UPPER LAYER	HMA PAVEMENT 3 MT 58-28 S	4 MT 58-28 H	508	2.5	QMP AS PER SS 460	ACCEPTANCE TESTING BY THE DEPARTMENT; NOT ELIGIBLE FOR INCENTIVE
271+12-1073+28	2X12 FT TRAVEL LANE	LOWER LAYER	MILLED/RELAYED EXISTING HMA SURFACE	3 MT 58-28 S	26,953	2.25	PWL INCENTIVE AIR VOIDS HMA PAVEMENT 460.2010	INCENTIVE DENSITY PWL HMA PAVEMENT 460.2005
271+12-1073+28	2X12 FT TRAVEL LANE	UPPER LAYER	HMA PAVEMENT 3 MT 58-28 S	4 MT 58-28 S	20,963	1.75	PWL INCENTIVE AIR VOIDS HMA PAVEMENT 460.2010	INCENTIVE DENSITY PWL HMA PAVEMENT 460.2005
271+12-1073+28	2X3 FT SHOULDER	LOWER LAYER	MILLED/RELAYED EXISTING HMA SURFACE	3 MT 58-28 S	6,738	2.25	PWL INCENTIVE AIR VOIDS HMA PAVEMENT 460.2010	ACCEPTANCE TESTING BY THE DEPARTMENT; NOT ELIGIBLE FOR INCENTIVE
271+12-1073+28	2X3 FT SHOULDER	UPPER LAYER	HMA PAVEMENT 3 MT 58-28 S	4 MT 58-28 S	5,241	1.75	PWL INCENTIVE AIR VOIDS HMA PAVEMENT 460.2010	ACCEPTANCE TESTING BY THE DEPARTMENT; NOT ELIGIBLE FOR INCENTIVE

CULVERT SUMMARY

CATEGORY	STATION	LOCATION	CULVERT PIPE REINFORCEMENT CONCRETE CLASS III 24-INCH 522.0124 LF	APRON ENDWALLS FOR CULVERT PIPE REINFORCEMENT CONCRETE 24-INCH 522.1024 EACH	CULVERT PIPE REINFORCEMENT CONCRETE HORIZONTAL ELLIPTICAL CLASS HE-III 43x68-INCH 522.2343 LF	APRON ENDWALLS FOR CULVERT PIPE REINFORCEMENT CONCRETE HORIZONTAL ELLIPTICAL 43x68-INCH 522.2643 EACH	MARKERS CULVERT END 633.5200 EACH	REMARKS
0010	327+56	STH 80	45	2	-	-	2	
0010	500+38	STH 80	50	2	-	-	2	
0010	1068+17	STH 80	50	2	-	-	2	
	0010 SUBTOTAL=		**145**	**6**	**0**	**0**	**6**	
0020	747+49	STH 80	-	-	64	2	2	INV. 933.26 FOR ALL PIPES.
0020	747+59	STH 80	-	-	64	2	2	2 CULVERT MARKERS REQ'D.
0020	747+69	STH 80	-	-	64	2	2	
	0020 SUBTOTAL=		**0**	**0**	**192**	**6**	**6**	
	PROJECT TOTALS =		**145**	**6**	**192**	**6**	**12**	

RUMBLE STRIPS

CATEGORY	STATION	TO	STATION	LOCATION	465.0475 LF
0010	271+12	-	283+59	MAINLINE	1,247
0010	288+15	-	339+72	MAINLINE	5,157
0010	343+72	-	396+10	MAINLINE	5,238
0010	400+10	-	507+12	MAINLINE	10,702
0010	511+12	-	562+75	MAINLINE	5,163
0010	566+75	-	590+56	MAINLINE	2,381
0010	594+56	-	701+02	MAINLINE	10,646
0010	705+02	-	785+07	MAINLINE	8,005
0010	789+07	-	886+87	MAINLINE	9,780
0010	890+87	-	939+90	MAINLINE	4,903
0010	943+90	-	993+00	MAINLINE	4,910
0010	997+00	-	1046+20	MAINLINE	4,920
0010	1050+20	-	1071+14	MAINLINE	2,094
			PROJECT TOTAL =		**75,146**

STORM SEWER SUMMARY

CATEGORY	STATION	LOCATION	RECONSTRUCTING INLETS 611.0430 EACH	INLET COVERS TYPE H 611.0624 EACH
0010	231+10	RT	1	1
0010	241+63	RT	1	1
0010	243+71	RT	1	1
0010	243+72	LT	1	1
0010	243+96	LT	1	1
0010	244+31	LT	1	1
0010	246+65	LT	1	1
0010	246+73	RT	1	1
0010	246+81	RT	1	1
0010	250+65	RT	1	1
0010	250+65	LT	1	1
0010	252+75	LT	1	1
0010	254+33	RT	1	1
0010	255+63	RT	1	1
0010	255+63	LT	1	1
0010	255+70	LT	1	1
0010	257+81	LT	1	1
0010	263+05	LT	1	1
0010	263+16	LT	1	1
0010	263+16	RT	1	1
0010	265+52	LT	1	1
0010	265+63	LT	1	1
0010	266+45	RT	1	1
0010	267+05	LT	1	1
0010	268+05	RT	1	1
	PROJECT TOTALS=		**25**	**25**

ADJUSTING MANHOLE COVERS

CATEGORY	STATION	LOCATION	611.8110 EACH	REMARKS
0010	241+59	7' RT	1	STORM SEWER
0010	243+69	7' RT	1	STORM SEWER
0010	246+65	6' RT	1	STORM SEWER
0010	255+61	5' RT	1	STORM SEWER
0010	257+79	9' RT	1	STORM SEWER
0010	261+23	6' RT	1	STORM SEWER
0010	263+16	5' RT	1	STORM SEWER
0010	265+64	8' LT	1	STORM SEWER
	PROJECT TOTAL =		**8**	

3

WATER

CATEGORY	LOCATION	624.0100 MGAL	REMARKS
0010	STH 80	1,084	MILL & RELAY
0010	STH 80	130	3/4-INCH
PROJECT TOTAL =		**1,214**	

MOBILIZATION EROSION CONTROL ITEMS

CATEGORY	LOCATION	STATION	TO	STATION	MOBILIZATIONS EROSION CONTROL 628.1905 EACH	MOBILIZATIONS EMERGENCY EROSION CONTROL 628.1910 EACH	REMARKS
0010	PROJECT	UNDISTRIBUTED			4	3	
PROJECT TOTALS =					**4**	**3**	

3

EROSION CONTROL ITEMS

CATEGORY	LOCATION	STATION	TO	STATION	SILT FENCE 628.1504 LF	SILT FENCE MAINENANCE 628.1520 LF	INLET PROTECTION TYPE C 628.7015 EACH	TEMPORARY DITCH CHECKS 628.7504 LF	CULVERT PIPE CHECKS 628.7555 EACH	REMARKS
0010	STH 80	SIDEWALK/CURB	UNDISTRIBUTED		-	-	33	-	-	
0010	STH 80	WAYSIDE REMOVAL			4,847	4,847	-	170	-	
0010	STH 80	327+56			-	-	-	-	8	CULVERT REPLACEMENT
0010	STH 80	500+38			-	-	-	-	8	CULVERT REPLACEMENT
0010	STH 80	747+59			400	400	-	40	24	CULVERT REPLACEMENT
0010	STH 80	1068+17			-	-	-	-	8	CULVERT REPLACEMENT
0010	PROJECT	UNDISTRIBUTED			525	525	-	21	6	
PROJECT TOTALS =					**5,772**	**5,772**	**33**	**231**	**54**	

FINISHING ITEMS

CATEGORY	LOCATION	STATION	TO	STATION	TOPSOIL 625.0100 SY	SALVAGED TOPSOIL 625.0500 SY	MULCHING 627.0200 SY	EROSION MAT URBAN CLASS 1 TYPE A 628.2006 SY	FERTILIZER TYPE B 629.0210 CWT	SEEDING MIXTURE NO. 40 630.0140 LB	SEED WATER 630.0500 MGAL	REMARKS
0010	STH 80	SIDEWALK/CURB	UNDISTRIBUTED		-	358	-	358	0.2	6.4	5	
0010	STH 80	WAYSIDE REMOVAL			3,490	5,870	3,490	5,870	5.9	168.4	66	
0010	STH 80	CULVERT REPLACEMENT			-	905	-	905	0.6	16.3	11	
0010	PROJECT	UNDISTRIBUTED			350	707	350	707	0.7	19.8	9	
PROJECT TOTALS =					**3,840**	**7,840**	**3,840**	**7,840**	**7.4**	**210.9**	**91**	

3

MOVING SIGNS SUMMARY

CATEGORY	LOCATION	STATION	MOVING SIGNS TYPE II 638.2102 EACH	REMOVING SMALL SIGN SUPPORTS 638.3000 EACH	POSTS WOOD 4X6-INCH X 16-FT 634.0616 EACH	REMARKS
0010	PROJECT	UNDISTRIBUTED	12	12	12	NO PASSING ZONE
PROJECT TOTALS =			**12**	**12**	**12**	

TRAFFIC CONTROL SUMMARY

CATEGORY	LOCATION	TRAFFIC CONTROL BARRICADES TYPE III 643.0420 DAY	TRAFFIC CONTROL WARNING LIGHTS TYPE A 643.0705 DAY	TRAFFIC CONTROL SIGNS 643.0900 DAY	TRAFFIC CONTROL SIGNS PCMS 643.1050 DAY	STAGE
0010	STH 173/STH 80 INT	40	80	140	-	DETOUR
0010	BOP	140	200	3	7	DETOUR
0010	CTH F/STH 80	40	80	60	-	DETOUR/SIDEROAD CLOSURE
0010	STH 21/STH 173	-	-	120	-	DETOUR
0010	CTH PP/STH 21	-	-	100	-	DETOUR
0010	CTH H/STH 21	-	-	100	-	DETOUR
0010	CTH M/STH 21	-	-	100	-	DETOUR
0010	STH 21/STH 80 INT	40	80	320	7	DETOUR
0010	DETOUR ROUTE	-	-	320	-	DETOUR
0010	NECEDAH SIDEROADS	480	960	480	-	DETOUR/SIDEROAD CLOSURES
0010	CULVERT REPLACEMENT	120	240	40	-	DETOUR/TYPE III AT CULV. REPL.
0010	SIDEWALK UNDISTRIBUTED	-	-	320	-	SIDEWALK CLOSURES
0010	BOP/EOP	-	-	800	-	ADVANCED WARNING
0010	CTH F & NECEDAH SIDEROADS	-	-	1,300	-	ADVANCED WARNING
PROJECT TOTAL =		**860**	**1,640**	**2,903**	**14**	

TRAFFIC CONTROL COVERING SIGNS TYPE II

CATEGORY	LOCATION	643.0920 EACH	NUMBER OF CYCLES	NUMBER OF SIGNS	STAGE
0010	BOP	3	1	3	DETOUR
0010	STH 21/STH 80 INT	6	1	6	DETOUR
PROJECT TOTAL =		**9**			

TEMPORARY PEDESTRIAN ITEMS

CATEGORY	LOCATION	TEMPORARY PEDESTRIAN SURFACE 644.1430 SF	TEMPORARY PEDESTRIAN CURB RAMP 644.1601 DAY	TEMPORARY PEDESTRIAN BARRICADE 644.1810 LF
0010	HIGH ST	472	21	-
0010	DIVISION ST	513	126	-
0010	PLUM ST	371	168	-
0010	BIRCH ST	-	42	-
0010	MAPLE ST	134	168	-
0010	KING ST	-	126	-
0010	HICKORY ST	-	42	-
0010	MIDDLE ST	431	168	-
0010	HARVEY ST	369	126	-
0010	NORTH ST	287	42	-
0010	UNDISTRIBUTED	-	-	300
PROJECT TOTALS		**2,577**	**1,029**	**300**

LOCATING NO-PASSING ZONES

CATEGORY	STATION	TO	STATION	LOCATION	648.0100 MI
0010	271+12	-	1073+28		15.2
PROJECT TOTAL =					**15.2**

PAVEMENT MARKING SUMMARY

CATEGORY	STATION		TO STATION	LOCATION	MARKING LINE EPOXY 4-INCH 646.1020 LF	MARKING STOP LINE EPOXY 18-INCH 646.6120 LF	MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH 646.7420 LF	TEMPORARY MARKING LINE PAINT 4-INCH 649.0105 LF	TEMPORARY MARKING LINE EPOXY 4-INCH 649.0120 LF	TEMPORARY MARKING LINE REMOVABLE TAPE 4-INCH 649.0150 LF	REMARKS
0010	230+32	-	240+72	BOP TO BIRCH ST	2,080	-	-	4,160	-	-	DOUBLE CENTERLINE - YELLOW
0010	240+72	-	247+27	BIRCH ST TO MAPLE ST	819	-	-	1,415	-	-	SOLID/DASHED CENTERLINE - YELLOW
0010	247+27	-	252+77	MAPLE ST TO KING ST	138	-	-	88	-	-	DASHED CENTERLINE - YELLOW
0010	252+77	-	259+42	KING ST TO MIDDLE ST	831	-	-	1,437	-	-	DASHED/SOLID CENTERLINE - YELLOW
0010	259+42		271+12	MIDDLE ST TO RURAL SECTION	2,340	-	-	4,680	-	-	DOUBLE CENTERLINE - YELLOW
0010	271+12	-	285+58	RURAL SECTION TO 12TH AVE	2,892	-	-	5,784	2,892	-	DOUBLE CENTERLINE - YELLOW
0010	285+58	-	291+38	12TH AVE TO END OF NPZ	725	-	-	1,253	626	-	SOLID/DASHED CENTERLINE - YELLOW
0010	291+38	-	642+92	END OF NPZ TO WAYSIDE	8,789	-	-	5,625	2,812	-	DASHED CENTERLINE - YELLOW
0010	642+92	-	658+50	WAYSIDE	19,485	-	-	38,970	19,485	-	DASHED/SOLID CENTERLINE - YELLOW
0010	658+50	-	669+20	WAYSIDE TO NB NPZ	268	-	-	171	86	-	DASHED CENTERLINE - YELLOW
0010	669+20	-	683+35	NB NPZ TO NB/SB NPZ	1,769	-	-	3,056	1,528	-	DASHED/SOLID CENTERLINE - YELLOW
0010	683+35	-	688+90	NB/SB NPZ TO SB NPZ	1,110	-	-	2,220	1,110	-	DOUBLE CENTERLINE - YELLOW
0010	688+90	-	702+85	SB NPZ TO 12TH AVE	1,744	-	-	3,013	1,507	-	SOLID/DASHED CENTERLINE - YELLOW
0010	702+85	-	1073+28	12TH AVE TO EOP	9,261	-	-	5,927	2,963	-	DASHED CENTERLINE - YELLOW
0010	271+12	-	285+74	JOHN ST TO 19TH ST W	1,471	-	-	-	-	-	LANE EDGE LT - WHITE
0010	271+12	-	285+74	JOHN ST TO 12TH AVE	1,377	-	-	-	-	-	LANE EDGE RT - WHITE
0010	285+74	-	341+30	19TH ST W TO 18TH ST W	5,489	-	-	-	-	-	LANE EDGE LT - WHITE
0010	285+74	-	341+30	12TH AVE TO 18TH ST W	5,510	-	-	-	-	-	LANE EDGE RT - WHITE
0010	341+30	-	397+80	18TH ST W TO 17TH ST W	5,572	-	-	-	-	-	LANE EDGE LT - WHITE
0010	341+30	-	397+80	18TH ST W TO 17TH ST W	5,560	-	-	-	-	-	LANE EDGE RT - WHITE
0010	397+80	-	592+26	17TH ST W TO 11TH ST W	19,340	-	-	-	-	-	LANE EDGE LT - WHITE
0010	397+80	-	508+72	17TH ST W TO 14TH ST W	11,053	-	-	-	-	-	LANE EDGE RT - WHITE
0010	508+72	-	564+40	14TH ST W TO 12TH ST	5,497	-	-	-	-	-	LANE EDGE RT - WHITE
0010	564+40	-	592+26	12TH ST TO 11TH ST W	2,703	-	-	-	-	-	LANE EDGE RT - WHITE
0010	592+26	-	702+42	11TH ST W TO 12TH AVE	10,952	-	-	-	-	-	LANE EDGE LT - WHITE
0010	592+26	-	702+42	11TH ST W TO 12TH AVE	10,953	-	-	-	-	-	LANE EDGE RT - WHITE
0010	702+42	-	786+68	12TH AVE TO E 7TH ST	8,302	-	-	-	-	-	LANE EDGE LT - WHITE
0010	702+42	-	888+60	12TH AVE TO 5TH ST E	18,503	-	-	-	-	-	LANE EDGE RT - WHITE
0010	786+68	-	888+60	E 7TH ST TO 5TH ST W	10,125	-	-	-	-	-	LANE EDGE LT - WHITE
0010	888+60	-	941+56	5TH ST W TO 4TH ST W	5,246	-	-	-	-	-	LANE EDGE LT - WHITE
0010	888+60	-	941+56	5TH ST E TO CTH F	5,207	-	-	-	-	-	LANE EDGE RT - WHITE
0010	941+56	-	994+72	4TH ST W TO 3RD ST	5,227	-	-	-	-	-	LANE EDGE LT - WHITE
0010	941+56	-	994+72	CTH F TO 3RD ST	5,211	-	-	-	-	-	LANE EDGE RT - WHITE
0010	994+72	-	1047+66	3RD ST TO 2ND ST	5,240	-	-	-	-	-	LANE EDGE LT - WHITE
0010	994+72	-	1047+66	3RD ST TO 2ND ST	5,243	-	-	-	-	-	LANE EDGE RT - WHITE
0010	1047+66	-	1073+28	2ND ST TO COUNTY LINE RD	2,453	-	-	-	-	-	LANE EDGE LT - WHITE
0010	1047+66	-	1073+28	2ND ST TO COUNTY LINE RD	2,448	-	-	-	-	-	LANE EDGE RT - WHITE
0010	232+87	-	233+33	HIGH ST. CROSSWALK	-	17	89	-	-	-	CROSSWALK & STOP LINE - WHITE
0010	236+78	-	237+26	N. DIVISION ST. CROSSWALK	-	18	88	-	-	-	CROSSWALK & STOP LINE - WHITE
0010	239+19	-	239+61	PLUM ST. CROSSWALK	-	16	154	-	-	-	CROSSWALK & STOP LINE - WHITE
0010	240+20	-	240+79	BIRCH ST. CROSSWALK	-	20	126	-	-	-	CROSSWALK & STOP LINE - WHITE
0010	243+84	-	244+34	MAPLE ST. CROSSWALK	-	15	142	-	-	-	CROSSWALK & STOP LINE - WHITE
0010	253+15	-	253+99	KING ST. CROSSWALK	-	28	206	-	-	-	CROSSWALK & STOP LINE - WHITE
0010	254+53	-	254+93	HICKORY ST. CROSSWALK	-	15	73	-	-	-	CROSSWALK & STOP LINE - WHITE
0010	261+20	-	262+18	W. MIDDLE ST. CROSSWALK	-	26	224	-	-	-	CROSSWALK & STOP LINE - WHITE
0010	264+86	-	266+02	N. HARVEY ST. CROSSWALK	-	44	157	-	-	-	CROSSWALK & STOP LINE - WHITE
0010	266+62	-	267+01	NORTH ST. CROSSWALK	-	16	75	-	-	-	CROSSWALK & STOP LINE - WHITE
0010	269+69	-	270+07	JOHN ST.	-	26	-	-	-	-	STOP LINE - WHITE
0010				STH 80 TEMP CROSSINGS	-	-	-	940	-	460	SEE TRAFFIC CONTROL PLAN
				PROJECT TOTALS =	**210,933**	**241**	**1,334**	**78,739**	**33,010**	**460**	

NOTE: TEMP. MARKING LINE PAINT PLACED IN 2 APPLICATIONS (BINDER AND FINAL SURFACE) IN URBAN SECTION
NOTE: TEMP. MARKING LINE PAINT PLACED IN 2 APPLICATIONS (MILLED AND BINDER) IN RURAL SECTION
NOTE: TEMP. MARKING LINE EPOXY PLACED IN 1 APPLICATION (FINAL SURFACE) FOR RUMBLE STRIP APPLICATION IN RURAL SECTION
NOTE: TEMP. MARKING LINE REMOVABLE TAPE PLACED ON FINAL SURFACE FOR TEMPORARY CROSSWALK

CONSTRUCTION STAKING SUMMARY

CATEGORY	STATION	TO	STATION	LOCATION	CONSTRUCTION STAKING CURB GUTTER AND CURB & GUTTER 650.5500 LF	CONSTRUCTION STAKING PIPE CULVERTS 650.6000 EACH	CONSTRUCTION STAKING RESURFACING REFERENCE 650.8000 LF	CONSTRUCTION STAKING CURB RAMPS 650.9000 EACH	CONSTRUCTION STAKING SUPPLEMENTAL CONTROL (PROJECT) 01.1620-02-78 650.9911 EACH
0010	230+32	-	1073+28		1,178	6	84,296	28	1
				PROJECT TOTALS =	**1,178**	**6**	**84,296**	**28**	**1**

ADJUSTING SANITARY MANHOLE COVERS

CATEGORY	STATION	OFFSET	LOCATION	SPV.0060.01 EACH
0030	231+35	11'	RT	1
0030	233+14	14'	RT	1
0030	240+26	5'	RT	1
0030	265+48	17'	LT	1
			PROJECT TOTAL =	**4**

SAWING ASPHALT

CATEGORY	STATION	TO	STATION	LOCATION	690.0150 LF	REMARKS
0010	PROJECT LIMITS			STH 80	1,178	AT CURB & GUTTER REMOVAL LOCATIONS
0010	648+97	-	663+10	WAYSIDE	1,595	
				PROJECT TOTAL=	**2,773**	

ADJUSTING WATER VALVES

CATEGORY	STATION	OFFSET	LOCATION	SPV.0060.02 EACH
0040	237+14	30'	LT	1
0040	237+30	30'	LT	1
0040	240+16	46'	LT	1
0040	244+08	22'	LT	3
0040	253+76	22'	LT	1
0040	258+51	13'	LT	1
0040	265+79	35'	RT	1
			PROJECT TOTAL=	**9**

SAWING CONCRETE

CATEGORY	STATION	TO	STATION	LOCATION	690.0250 LF	REMARKS
0010	232+86	-	942+14	VARIES	140	CURB REPLACEMENT
				PROJECT TOTAL =	**140**	

TEMPORARY WATER DIVERSION B-29-0073

CATEGORY	STATION	LOCATION	SPV.0060.03 EACH	REMARKS
0020	747+59	STH 80	1	B-29-0073
		CAT 0020 SUBTOTAL=	**1**	

BEGIN PROJECT
STA. 230+32

Y = 240531.913
X = 463308.100
MATCH EXISTING
SAWCUT REQ'D
BEGIN URBAN SECTION PAVING
MILL AND OVERLAY

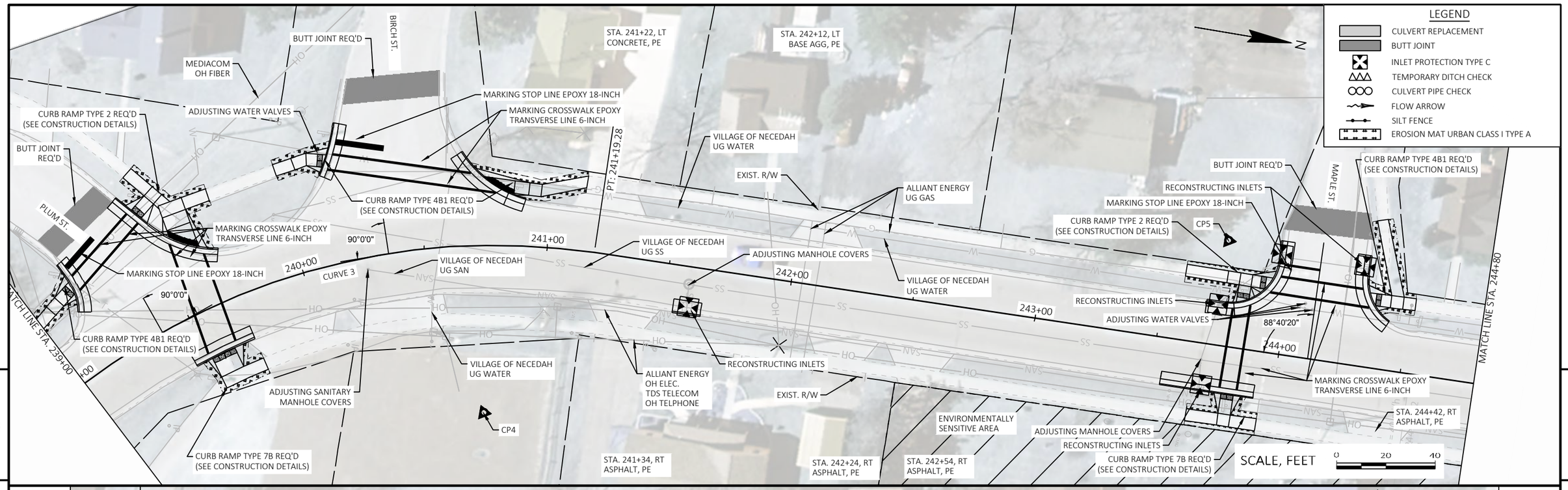
STH 21
BP: 227+43.70
STA 227+43.70
DESIGN SPEED = 30 mi/h
EXIST. R/W
PT: 230+10.82
VILLAGE OF NECEDAH UG WATER
228+00
229+00
230+00
231+00
232+00
CURVE 1
CP1
EXIST. R/W
ALLIANT ENERGY OH ELEC.
BUTT JOINT REQ'D STA. 230+32- 230+82
RECONSTRUCTING INLETS
ADJUSTING SANITARY MANHOLE COVERS
VILLAGE OF NECEDAH UG SAN
VILLAGE OF NECEDAH UG SS
STA. 231+70, LT ASPHALT, CE
MEDIACOM UG FIBER
PC: 232+02.55
BUTT JOINT REQ'D
HIGH ST.
CP2
ALLIANT ENERGY OH ELEC.
MARKING STOP LINE EPOXY 18-INCH
CURB RAMP TYPE 4B1 REQ'D (SEE CONSTRUCTION DETAILS)
MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH
90°0'0"
CURVE 2
233+00
ADJUSTING SANITARY MANHOLE COVERS
MATCH LINE STA. 233+40
N
SCALE, FEET 0 20 40

STA. 233+74, LT CONCRETE, PE
CURB RAMP TYPE 4B1 REQ'D (SEE CONSTRUCTION DETAILS)
ALLIANT ENERGY OH ELEC.
ALLIANT ENERGY UG GAS
PT: 234+77.11
EXIST. R/W
STA. 235+48, LT CONCRETE, PE
VILLAGE OF NECEDAH UG WATER
STA. 236+32, LT BASE AGG, PE
MARKING STOP LINE EPOXY 18-INCH
CURB RAMP TYPE 4B1 REQ'D (SEE CONSTRUCTION DETAILS)
N. DIVISION ST.
BUTT JOINT REQ'D
MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH
ALLIANT ENERGY OH ELEC. MEDIACOM OH FIBER
CURB RAMP TYPE 4B1 REQ'D (SEE CONSTRUCTION DETAILS)
PC: 238+54.63
MATCH LINE STA. 233+40
CURVE 2
234+00
235+00
236+00
237+00
238+00
CURVE 3
VILLAGE OF NECEDAH UG SS
73°32'45"
ADJUSTING WATER VALVES
CP3
ALLIANT ENERGY OH ELEC.
VILLAGE OF NECEDAH UG SAN
EXIST. R/W
MATCH LINE STA. 239+00
N
SCALE, FEET 0 20 40

LEGEND
- CULVERT REPLACEMENT
- BUTT JOINT
- INLET PROTECTION TYPE C
- TEMPORARY DITCH CHECK
- CULVERT PIPE CHECK
- FLOW ARROW
- SILT FENCE
- EROSION MAT URBAN CLASS I TYPE A

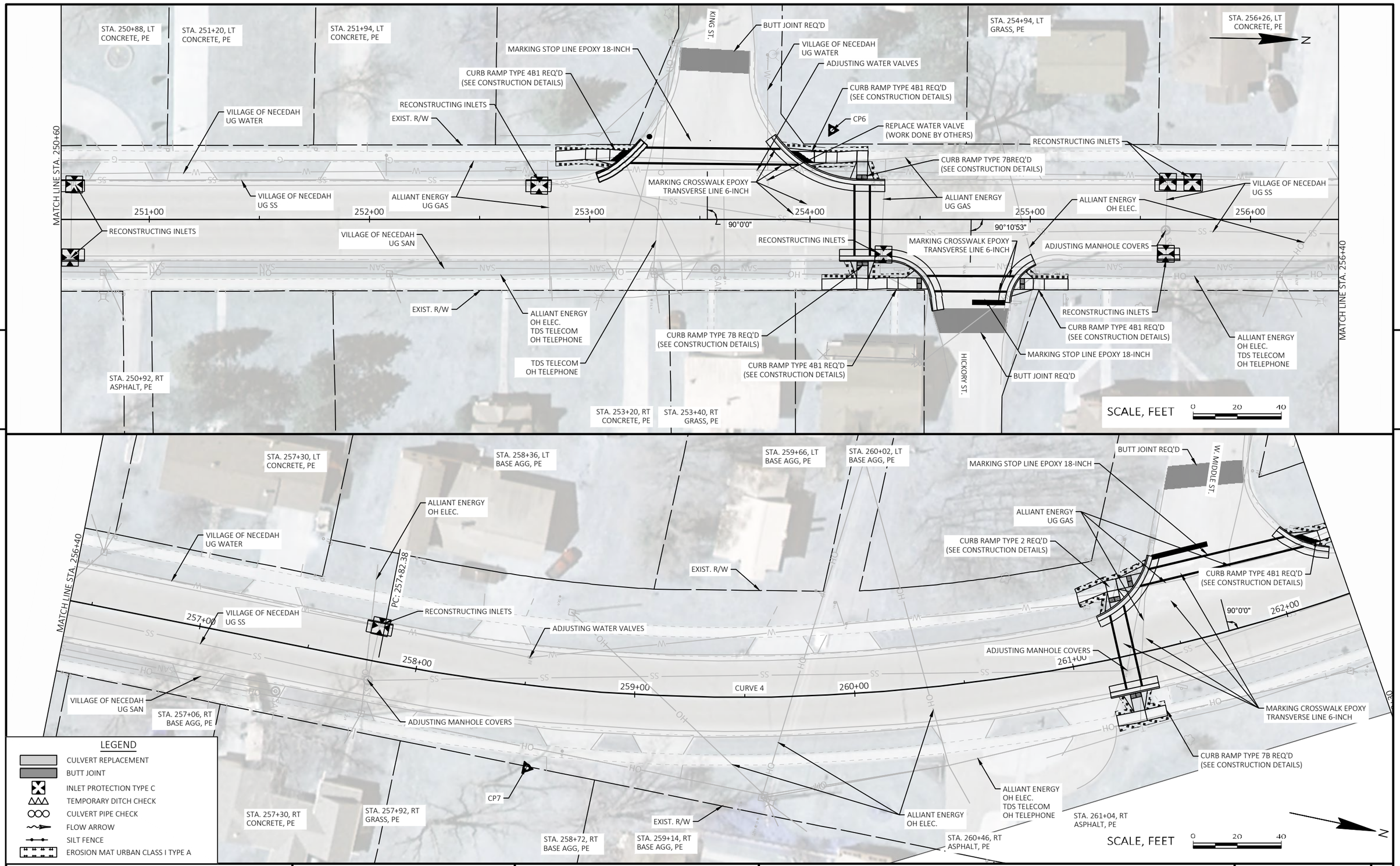
PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	PLAN	SHEET 51	E

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\050201_PN.DWG
LAYOUT NAME - Sheet - 1
PLOT DATE : 9/21/2022 2:35 PM
PLOT BY : MARZOLF, ROBBI
PLOT NAME :
PLOT SCALE : 1 IN:40 FT



PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	PLAN	SHEET 52 E

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\050201_PN.DWG
LAYOUT NAME - Sheet - 2
PLOT DATE : 9/21/2022 2:35 PM PLOT BY : MARZOLF, ROBBI PLOT NAME : PLOT SCALE : 1 IN:40 FT



PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	PLAN	SHEET 53	E

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\050201_PN.DWG
LAYOUT NAME - Sheet - 3

PLOT DATE : 9/21/2022 2:36 PM PLOT BY : MARZOLF, ROBBI PLOT NAME : PLOT SCALE : 1 IN:40 FT

WISDOT/CADDS SHEET 44

STA. 263+54, LT
GRASS, PE

N. HARVEY ST.

VILLAGE OF NECEDAH
UG WATER

BUTT JOINT REQ'D

MARKING STOP LINE EPOXY 18-INCH

CURB RAMP TYPE 4B1 REQ'D
(SEE CONSTRUCTION DETAILS)

BUTT JOINT REQ'D

MARKING STOP LINE EPOXY 18-INCH

NORTH ST.

CURB RAMP TYPE 4B1 REQ'D
(SEE CONSTRUCTION DETAILS)

RECONSTRUCTING INLETS

ALLIANT ENERGY
UG GAS

CURB RAMP TYPE 2 REQ'D
(SEE CONSTRUCTION DETAILS)

CP8

EXIST. R/W

PT: 266+03.28

CURB RAMP TYPE 4B1 REQ'D
(SEE CONSTRUCTION DETAILS)

RECONSTRUCTING INLETS

MARKING CROSSWALK EPOXY
TRANSVERSE LINE 6-INCH

MATCH LINE STA. 262+30

263+00

ADJUSTING MANHOLE COVERS

264+00

CURVE 4

MARKING CROSSWALK EPOXY
TRANSVERSE LINE 6-INCH

265+00

90°0'0"

94°02'46"

266+00

ADJUSTING MANHOLE COVERS

267+00

90°0'0"

VILLAGE OF NECEDAH
UG SS

RECONSTRUCTING INLETS

MATCH LINE STA. 268+00

RECONSTRUCTING INLETS

VILLAGE OF NECEDAH
UG SAN
ADJUSTING SANITARY MANHOLE COVERS

ALLIANT ENERGY
OH ELEC.

CURB RAMP TYPE 4B1 REQ'D
(SEE CONSTRUCTION DETAILS)

TDS TELECOM
UG TELEPHONE

CURB RAMP TYPE 4B1 REQ'D
(SEE CONSTRUCTION DETAILS)

EXIST. R/W

CURB RAMP TYPE 7B REQ'D
(SEE CONSTRUCTION DETAILS)

TDS TELEC.OM
OH TELEPHONE
ALLIANT ENERGY
OH ELEC.

TDS TELECOM
OH TELEPHONE
ALLIANT ENERGY
OH ELEC.

N. HARVEY ST.

MARKING STOP LINE EPOXY 18-INCH

ADJUSTING WATER VALVE

ST.

BUTT JOINT REQ'D

SCALE, FEET 0 20 40

5

5

STA. 268+48, LT
GRASS, PE

JOHN ST.

BUTT JOINT REQ'D

ALLIANT ENERGY
OH ELEC.

ENVIRONMENTALLY
SENSITIVE AREA

CURB RAMP TYPE 4B1 REQ'D
(SEE CONSTRUCTION DETAILS)

EXIST. R/W

CURB RAMP TYPE 4B1 REQ'D
(SEE CONSTRUCTION DETAILS)

IENTALLY
AREA

PC: 269+27.67

BUTT JOINT REQ'D (STA. 271+12- 271+62)
END URBAN SECTION PAVING
BEGIN RURAL SECTION PAVING
MILLING, MILL & RELAY, AND OVERLAY

MARKING STOP LINE EPOXY 18-INCH

MATCH LINE STA. 268+00

269+00

270+00

90°0'0"

271+00

CURVE 5

MATCH LINE STA. 272+00

RECONSTRUCTING INLETS

CP9

EXIST. R/W

SCALE, FEET 0 20 40

LEGEND

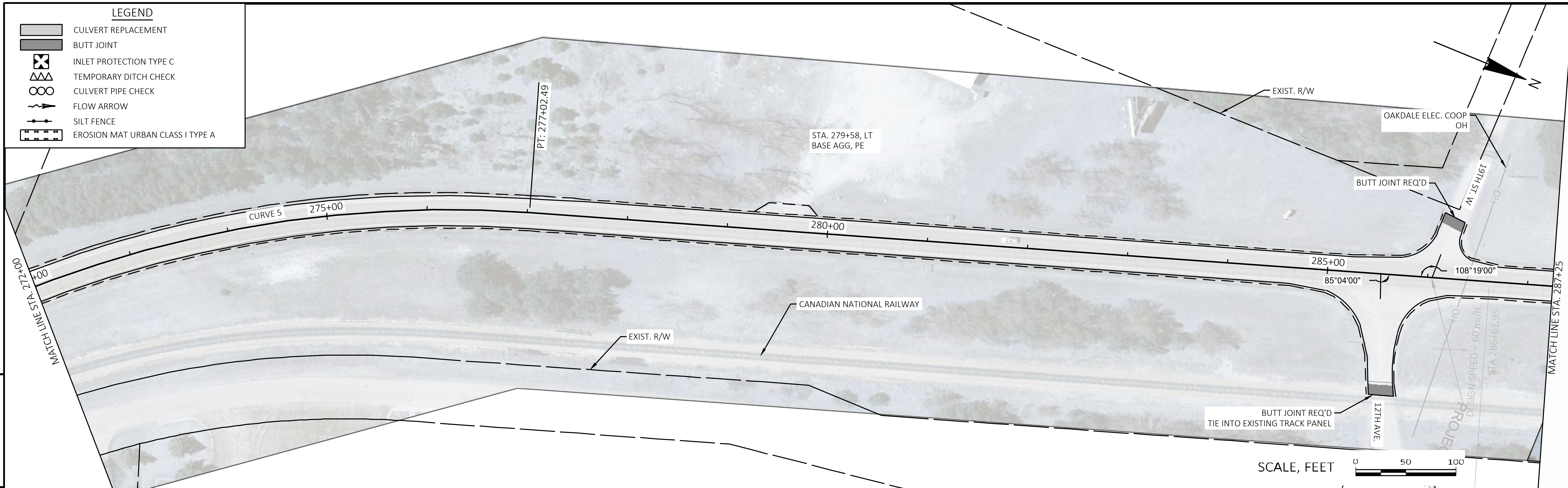
- CULVERT REPLACEMENT
- BUTT JOINT
- INLET PROTECTION TYPE C
- TEMPORARY DITCH CHECK
- CULVERT PIPE CHECK
- FLOW ARROW
- SILT FENCE
- EROSION MAT URBAN CLASS I TYPE A

PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	PLAN	SHEET 54	E

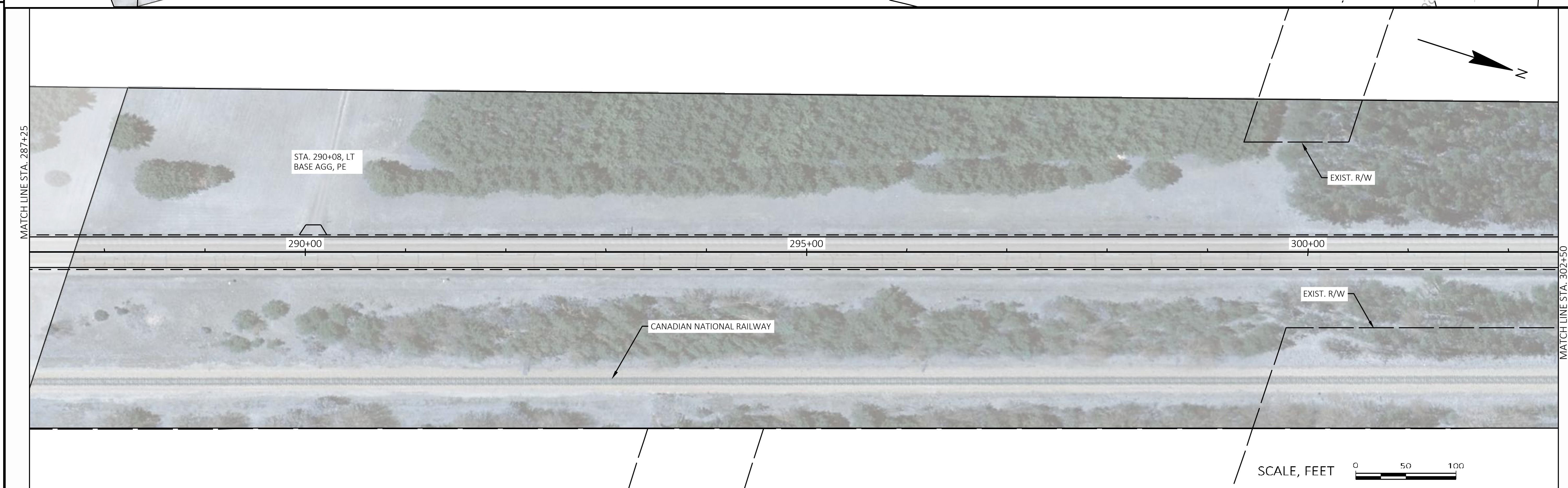
FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\050201_PN.DWG
LAYOUT NAME - Sheet - 4

PLOT DATE : 9/21/2022 2:36 PM PLOT BY : MARZOLF, ROBBI PLOT NAME : PLOT SCALE : 1 IN:40 FT

WISDOT/CADDS SHEET 44



LEGEND
CULVERT REPLACEMENT
BUTT JOINT
INLET PROTECTION TYPE C
TEMPORARY DITCH CHECK
CULVERT PIPE CHECK
FLOW ARROW
SILT FENCE
EROSION MAT URBAN CLASS I TYPE A
PT: 277+02.49
STA. 279+58, LT
BASE AGG, PE
EXIST. R/W
OAKDALE ELEC. COOP
OH
BUTT JOINT REQ'D
19TH ST. W
CURVE 5
275+00
280+00
285+00
108°19'00"
85°04'00"
MATCH LINE STA. 272+00
MATCH LINE STA. 287+25
CANADIAN NATIONAL RAILWAY
EXIST. R/W
BUTT JOINT REQ'D
TIE INTO EXISTING TRACK PANEL
12TH AVE.
DESIGN SPEED = 60 mi/h
STA 286+61.95
SCALE, FEET
0
50
100
5
5



MATCH LINE STA. 287+25
STA. 290+08, LT
BASE AGG, PE
EXIST. R/W
290+00
295+00
300+00
EXIST. R/W
CANADIAN NATIONAL RAILWAY
MATCH LINE STA. 302+50
SCALE, FEET
0
50
100

MATCH LINE STA. 302+50

STA. 313+88, LT
BASE AGG, PE

N

EXIST. R/W

305+00 310+00 315+00

ENVIRONMENTALLY SENSITIVE AREA

EXIST. R/W

CANADIAN NATIONAL RAILWAY

MATCH LINE STA. 317+75

5

SCALE, FEET 0 50 100

5

MATCH LINE STA. 317+75

STA. 327+28, LT
BASE AGG, PE

N

EXIST. R/W

CULVERT REPLACEMENT (STA. 327+04 TO 328+08)
STA 327+56,
REMOVING SMALL PIPE CULVERTS
CULVERT PIPE REINFORCED CONCRETE CLASS III 24-IN, 45 LF
APRON ENDWALLS FOR CULVERT PIPE REINFORCED CONCRETE 24-INCH, 2 EACH
MARKERS CULVERT END, 2 EACH

CP10

320+00 325+00 330+00

TDS UG TELEPHONE

CANADIAN NATIONAL RAILWAY

EXIST. R/W

MATCH LINE STA. 333+00

LEGEND

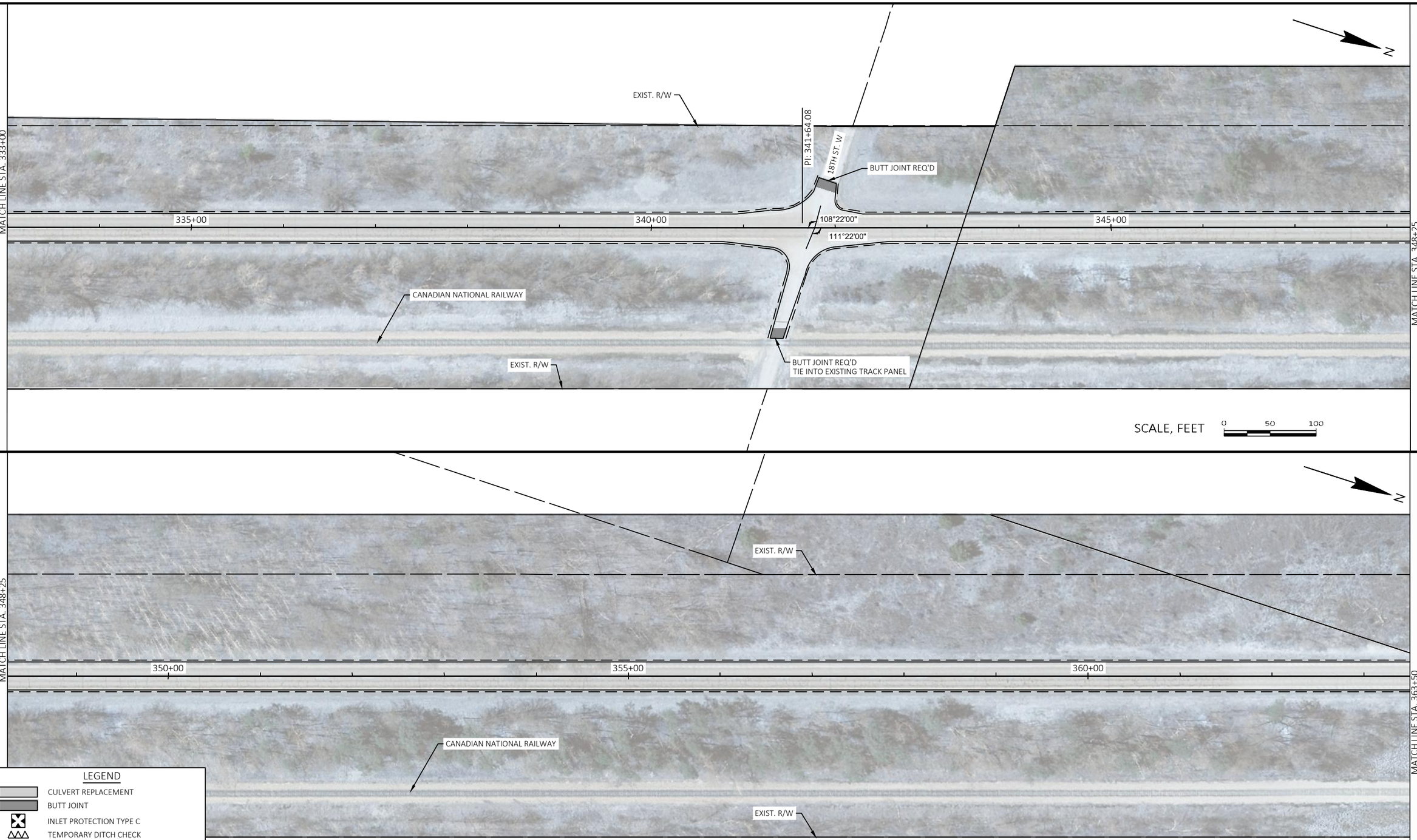
- CULVERT REPLACEMENT
- BUTT JOINT
- INLET PROTECTION TYPE C
- TEMPORARY DITCH CHECK
- CULVERT PIPE CHECK
- FLOW ARROW
- SILT FENCE
- EROSION MAT URBAN CLASS I TYPE A

SCALE, FEET 0 50 100

PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	PLAN	SHEET 56	E

FILE NAME : W:\7607\CAD\I 1620-02-08\SHEETSPLAN\050201_PN.DWG
LAYOUT NAME - Sheet - 6

PLOT DATE : 9/21/2022 2:36 PM PLOT BY : MARZOLF, ROBBI PLOT NAME : PLOT SCALE : 1 IN:100 FT



5

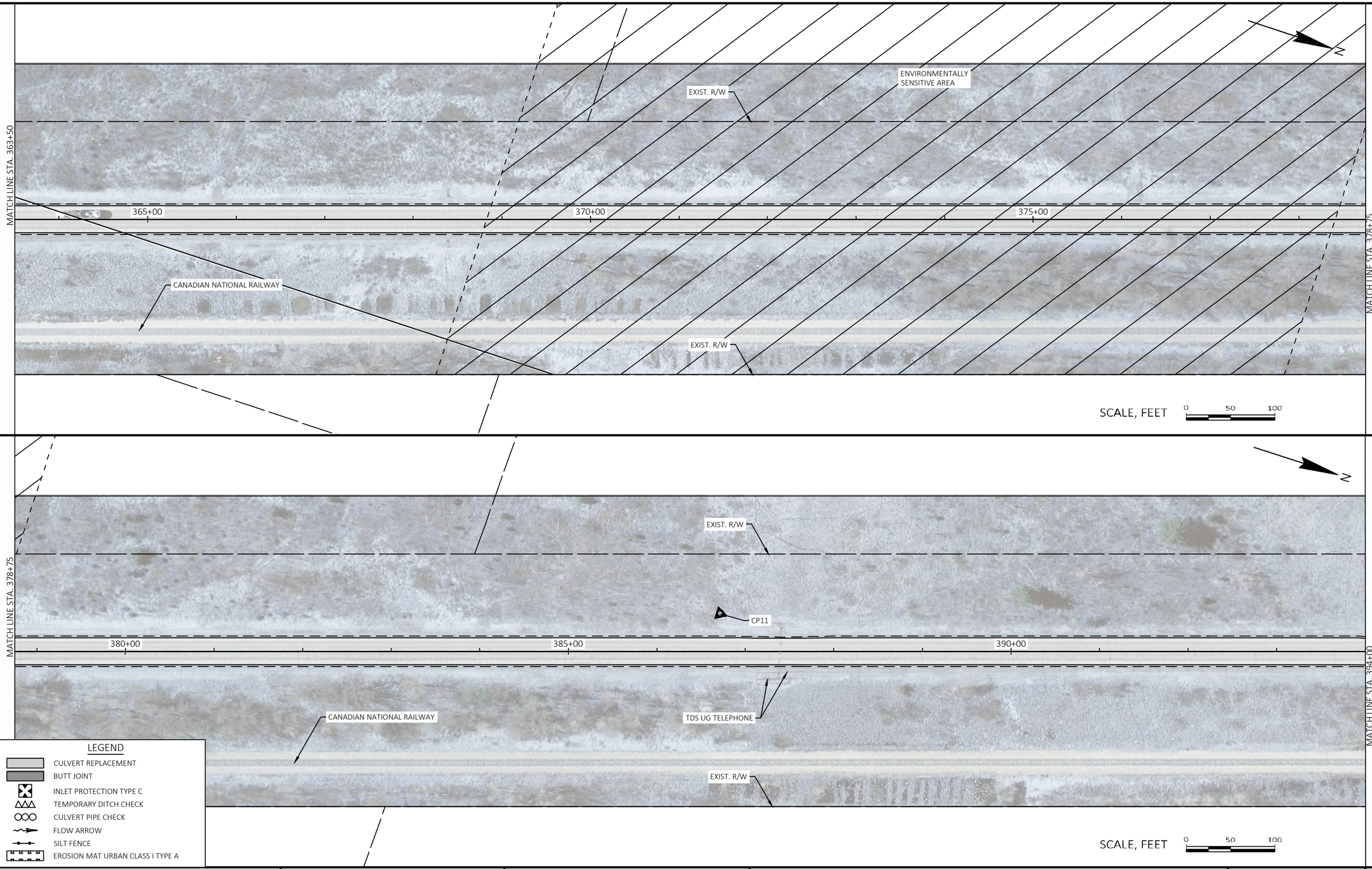
5

PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	PLAN	SHEET 57	E

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\050201_PN.DWG
LAYOUT NAME - Sheet - 7

PLOT DATE : 9/21/2022 2:36 PM PLOT BY : MARZOLF, ROBBI PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44



5

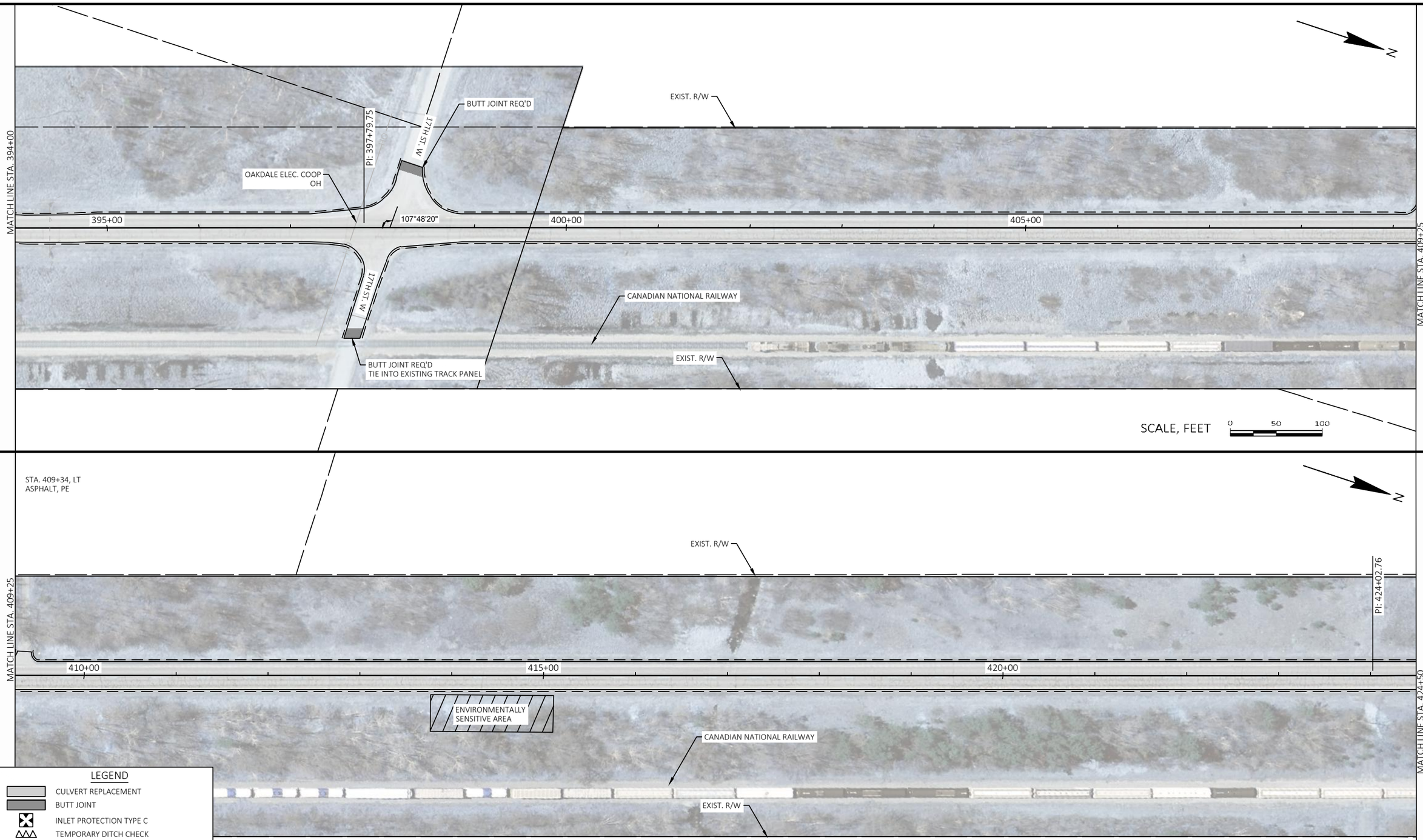
5

PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	PLAN	SHEET 58	E

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\050201_PN.DWG
LAYOUT NAME - Sheet - 8

PLOT DATE : 9/21/2022 2:36 PM PLOT BY : MARZOLF, ROBBI PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44



5

5

LEGEND

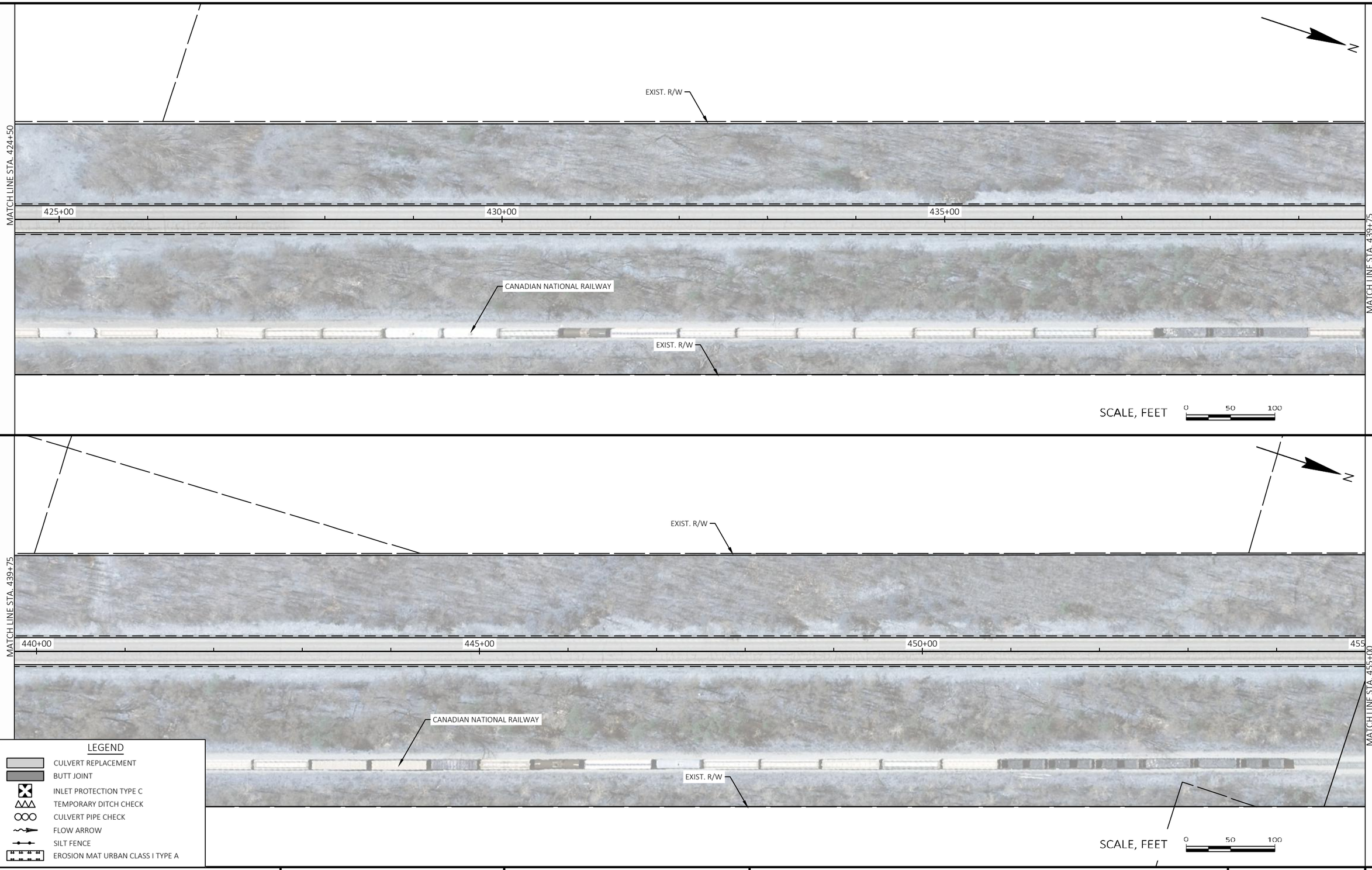
- CULVERT REPLACEMENT
- BUTT JOINT
- INLET PROTECTION TYPE C
- TEMPORARY DITCH CHECK
- CULVERT PIPE CHECK
- FLOW ARROW
- SILT FENCE
- EROSION MAT URBAN CLASS I TYPE A

PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	PLAN	SHEET 59	E

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\050201_PN.DWG
LAYOUT NAME - Sheet - 9

PLOT DATE : 9/21/2022 2:37 PM PLOT BY : MARZOLF, ROBBI PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44



5

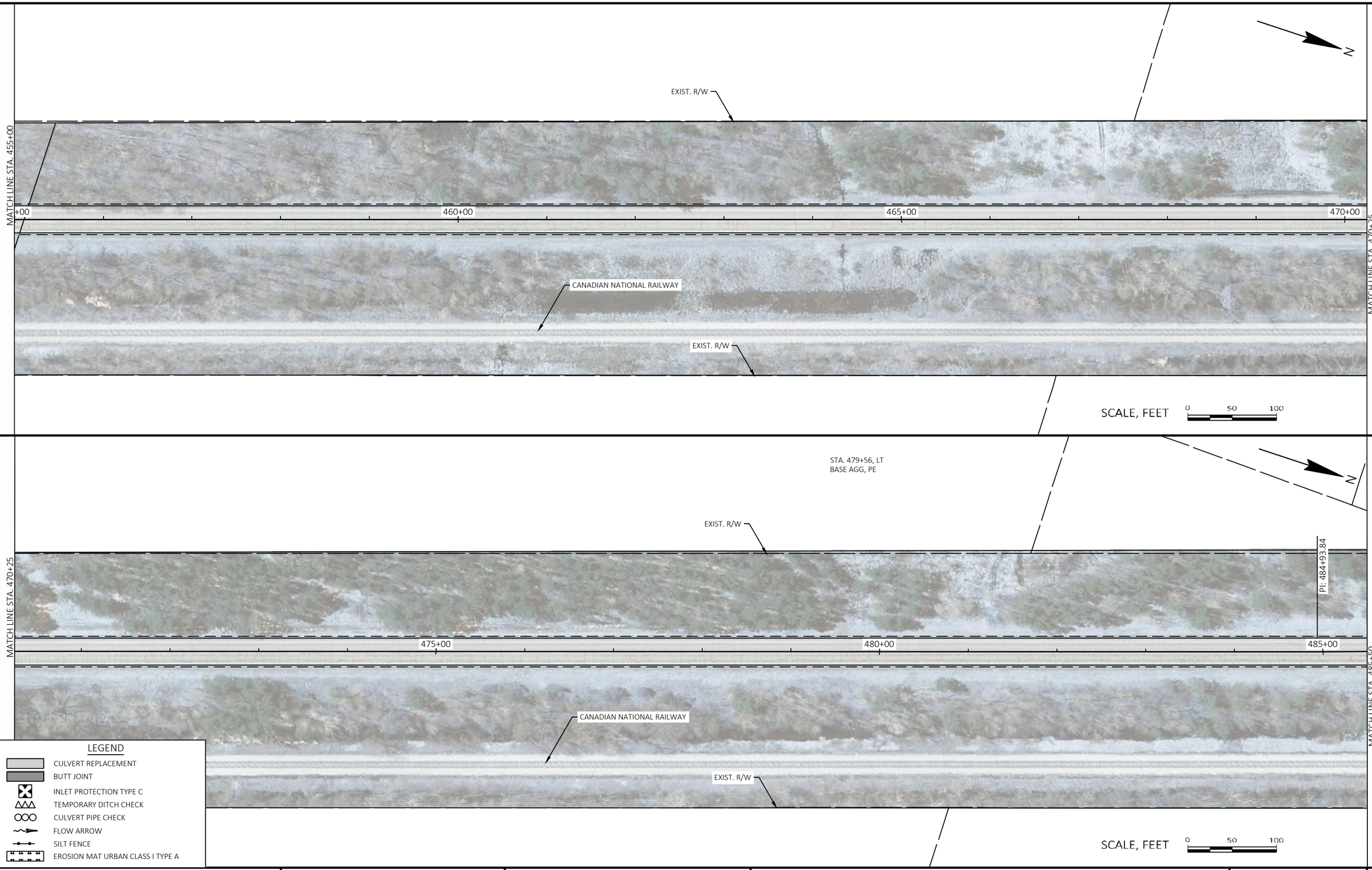
5

PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	PLAN	SHEET 60	E

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\050201_PN.DWG
LAYOUT NAME - Sheet - 10

PLOT DATE : 9/21/2022 2:37 PM PLOT BY : MARZOLF, ROBBI PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44



5

5

PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	PLAN	SHEET 61	E

STA. 493+28, LT
BASE AGG, PE

N

EXIST. R/W

MATCH LINE STA. 485+50

CP12

CULVERT REPLACEMENT (STA. 499+86 TO 500+90)
STA 500+38
REMOVING SMALL PIPE CULVERTS
CULVERT PIPE REINFORCED CONCRETE CLASS III 24-IN, 50 LF
APRON ENDWALLS FOR CULVERT PIPE REINFORCED CONCRETE 24-INCH, 2 EACH
MARKERS CULVERT END, 2 EACH

490+00

495+00

500+00

MATCH LINE STA. 501+75

CANADIAN NATIONAL RAILWAY

EXIST. R/W

5

5

SCALE, FEET 0 50 100

N

EXIST. R/W

PI: 509+44.09

505+00

510+00

515+00

105°24'40"

ENVIRONMENTALLY SENSITIVE AREA

14TH ST. W

CANADIAN NATIONAL RAILWAY

BUTT JOINT REQ'D
TIE INTO EXISTING TRACK PANEL

EXIST. R/W

MATCH LINE STA. 516+00

LEGEND

- CULVERT REPLACEMENT
- BUTT JOINT
- INLET PROTECTION TYPE C
- TEMPORARY DITCH CHECK
- CULVERT PIPE CHECK
- FLOW ARROW
- SILT FENCE
- EROSION MAT URBAN CLASS I TYPE A

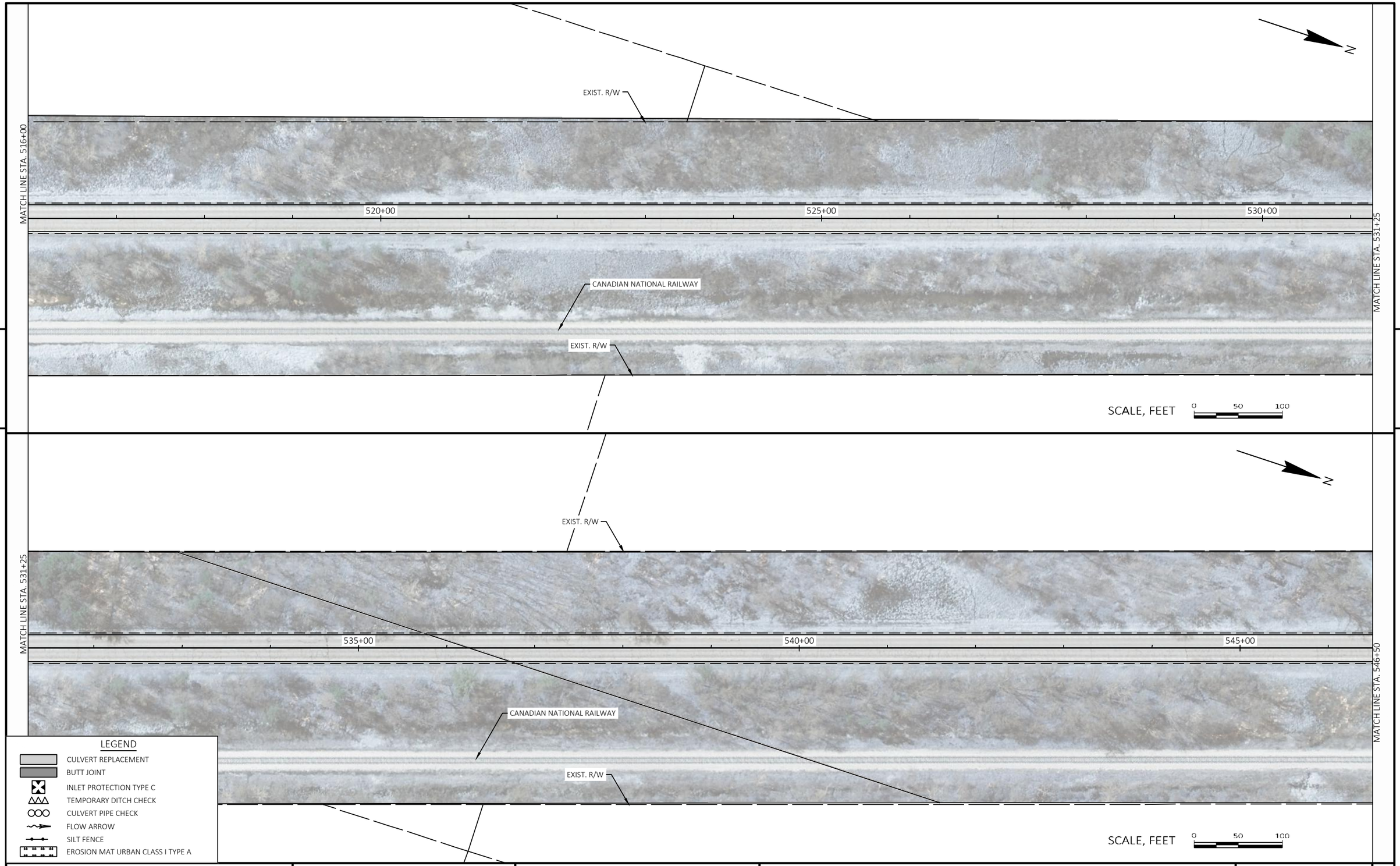
SCALE, FEET 0 50 100

PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	PLAN	SHEET 62	E

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\050201_PN.DWG
LAYOUT NAME - Sheet - 12

PLOT DATE : 9/21/2022 2:37 PM PLOT BY : MARZOLF, ROBBI PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44



5

5

PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	PLAN	SHEET 63	E

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\050201_PN.DWG
LAYOUT NAME - Sheet - 13

PLOT DATE : 9/21/2022 2:37 PM PLOT BY : MARZOLF, ROBBI PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44

STA. 546+80, LT
BASE AGG, PE

N

EXIST. R/W

MATCH LINE STA. 546+50

550+00 555+00 560+00

MATCH LINE STA. 561+75

EXIST. R/W

SCALE, FEET 0 50 100

5

5

N

EXIST. R/W

MATCH LINE STA. 561+75

565+00 570+00 575+00

105°24'40"

12TH ST

BUTT JOINT REQ'D
TIE INTO EXISTING TRACK PANEL

EXIST. R/W

MATCH LINE STA. 577+00

LEGEND

- CULVERT REPLACEMENT
- BUTT JOINT
- INLET PROTECTION TYPE C
- TEMPORARY DITCH CHECK
- CULVERT PIPE CHECK
- FLOW ARROW
- SILT FENCE
- EROSION MAT URBAN CLASS I TYPE A

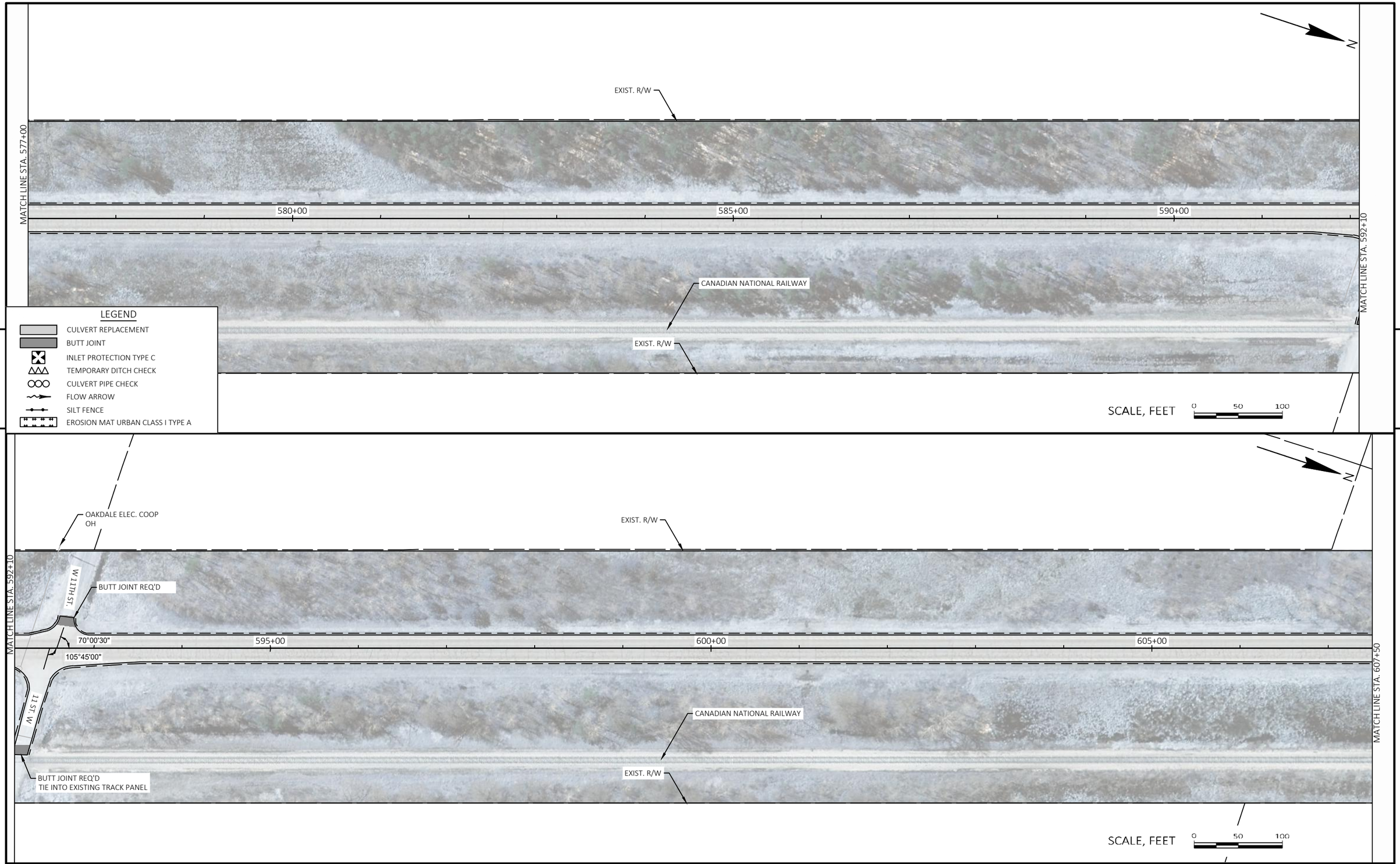
SCALE, FEET 0 50 100

PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	PLAN	SHEET 64	E

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\050201_PN.DWG
LAYOUT NAME - Sheet - 14

PLOT DATE : 9/21/2022 2:37 PM PLOT BY : MARZOLF, ROBBI PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44

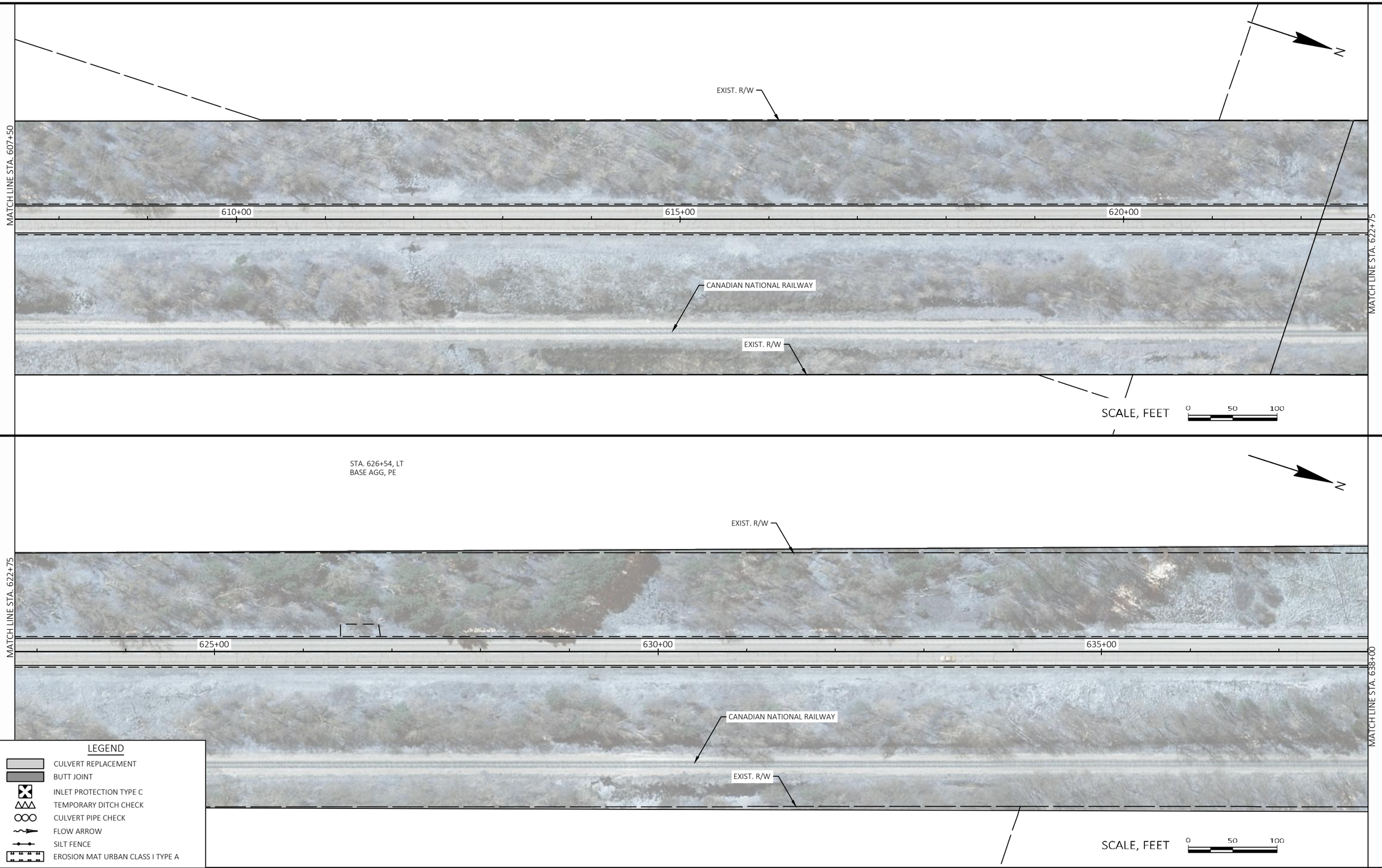


PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	PLAN	SHEET 65	E

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\050201_PN.DWG
LAYOUT NAME - Sheet - 15

PLOT DATE : 9/21/2022 2:38 PM PLOT BY : MARZOLF, ROBBI PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44



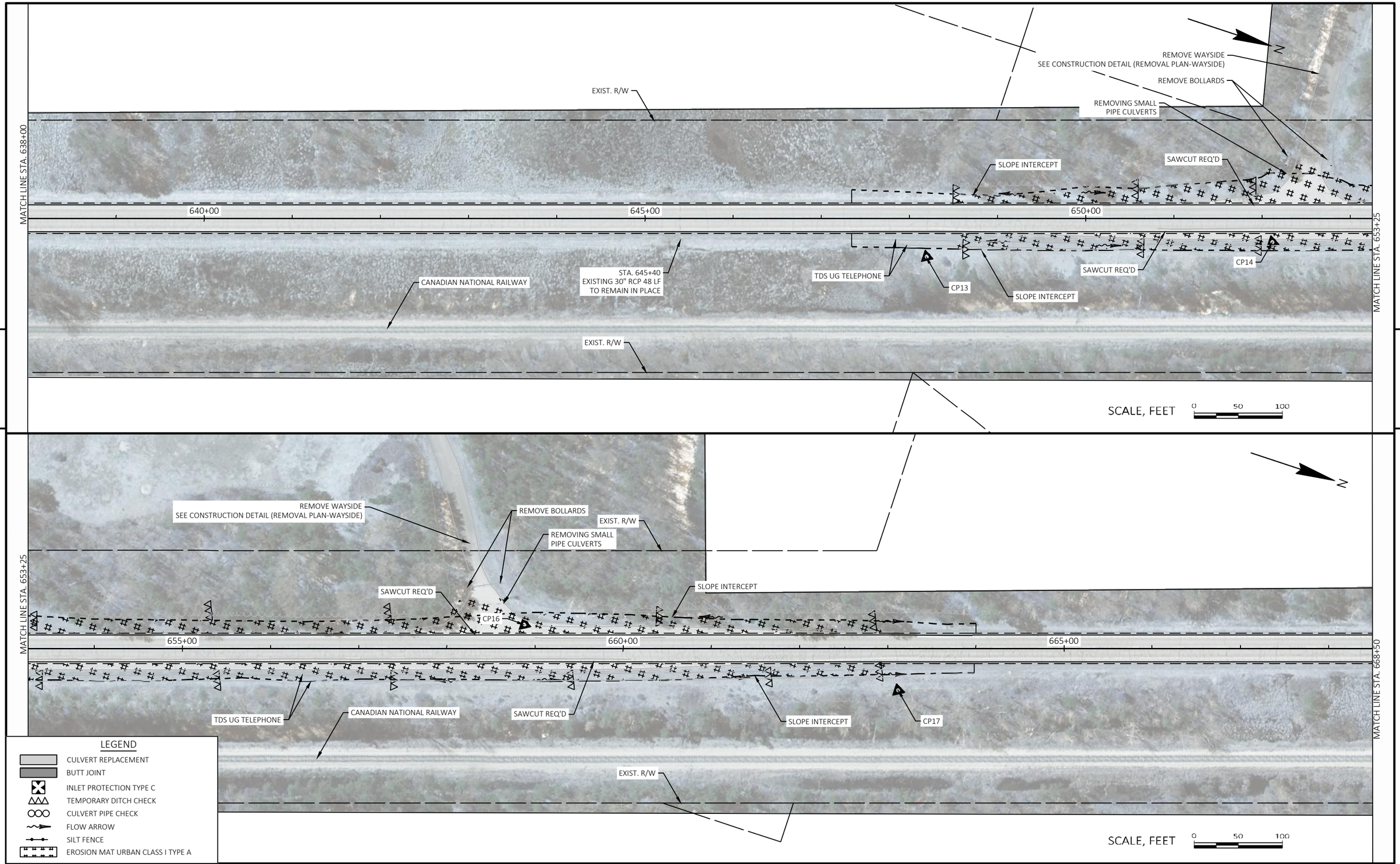
5

5

PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	PLAN	SHEET 66	E

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\050201_PN.DWG
LAYOUT NAME - Sheet - 16

PLOT DATE : 9/21/2022 2:38 PM PLOT BY : MARZOLF, ROBBI PLOT NAME : PLOT SCALE : 1 IN:100 FT



LEGEND

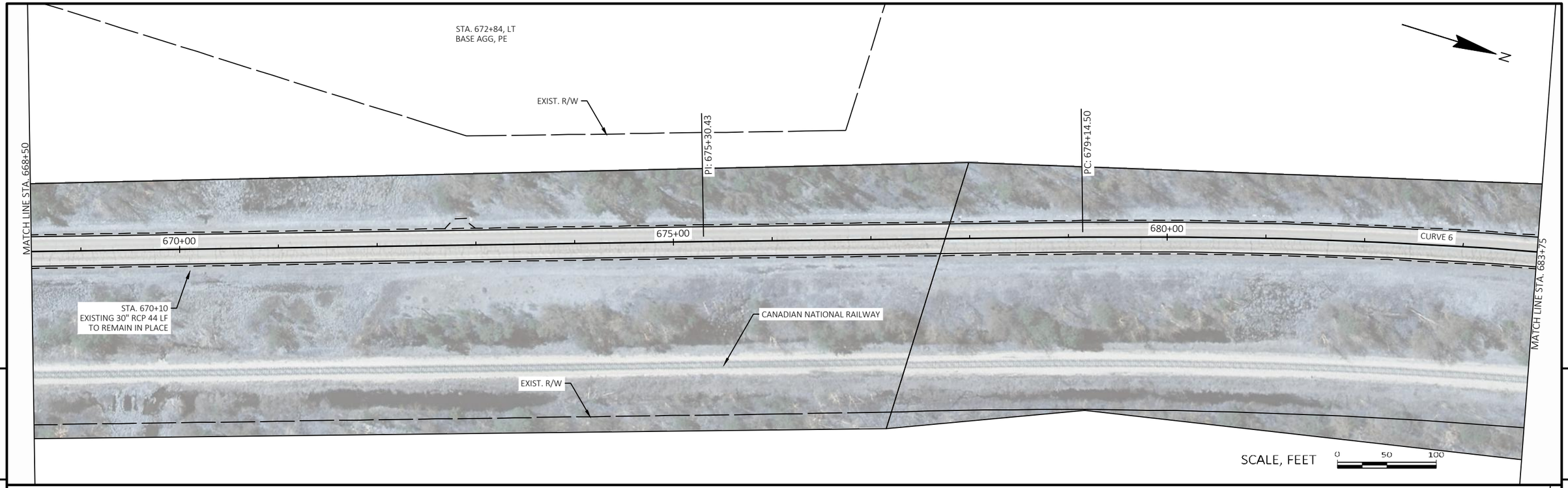
- CULVERT REPLACEMENT
- BUTT JOINT
- INLET PROTECTION TYPE C
- TEMPORARY DITCH CHECK
- CULVERT PIPE CHECK
- FLOW ARROW
- SILT FENCE
- EROSION MAT URBAN CLASS I TYPE A

PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	PLAN	SHEET 67	E

FILE NAME : W:\7607\CAD\I 1620-02-08\SHEETSPLAN\050201_PN.DWG
LAYOUT NAME - Sheet - 17

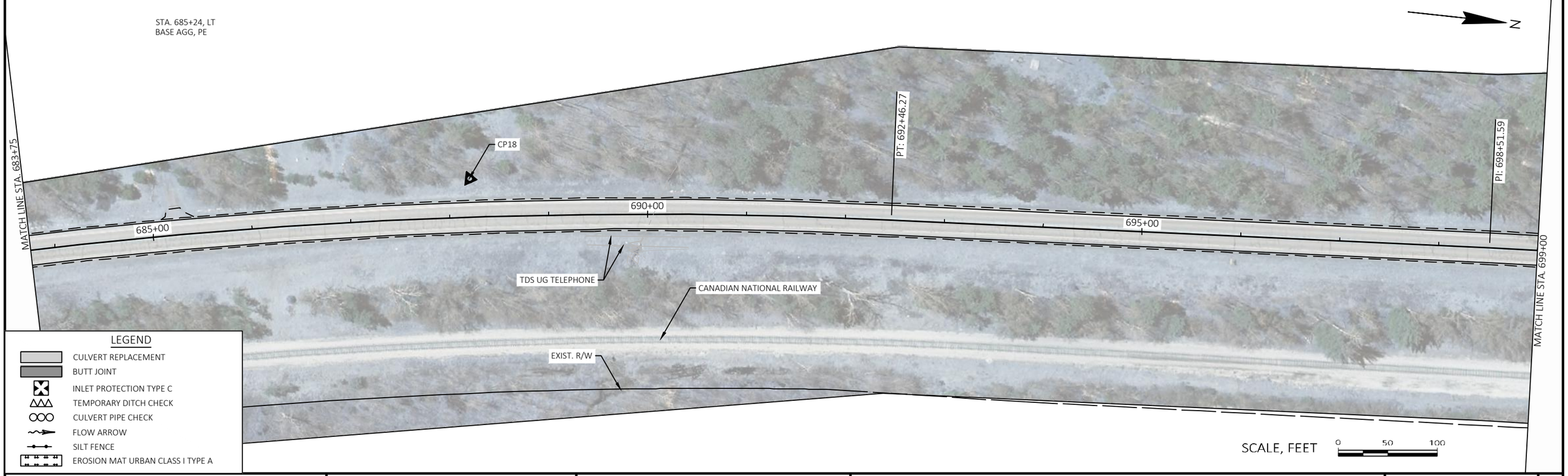
PLOT DATE : 9/21/2022 2:38 PM PLOT BY : MARZOLF, ROBBI PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44



5

5



PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	PLAN	SHEET 68	E

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\050201_PN.DWG
LAYOUT NAME - Sheet - 18

PLOT DATE : 9/21/2022 2:38 PM PLOT BY : MARZOLF, ROBBI PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44

STA. 706+28, LT
BASE AGG, PE

MATCH LINE STA. 699+00

BUTT JOINT REQ'D

9TH ST. W

EXIST. R/W

700+00

95°10'00"

92°42'30"

705+00

710+00

MATCH LINE STA. 714+25

ENVIRONMENTALLY SENSITIVE AREA

CANADIAN NATIONAL RAILWAY

OAKDALE ELEC. COOP OH

12TH AVE.

BUTT JOINT REQ'D
TIE INTO EXISTING TRACK PANEL

EXIST. R/W

5

5

SCALE, FEET 0 50 100

N

EXIST. R/W

MATCH LINE STA. 714+25

715+00

720+00

725+00

MATCH LINE STA. 729+50

CANADIAN NATIONAL RAILWAY

EXIST. R/W

LEGEND

- CULVERT REPLACEMENT
- BUTT JOINT
- INLET PROTECTION TYPE C
- TEMPORARY DITCH CHECK
- CULVERT PIPE CHECK
- FLOW ARROW
- SILT FENCE
- EROSION MAT URBAN CLASS I TYPE A

SCALE, FEET 0 50 100

PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	PLAN	SHEET 69	E

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\050201_PN.DWG
LAYOUT NAME - Sheet - 19

PLOT DATE : 9/21/2022 2:38 PM PLOT BY : MARZOLF, ROBBI PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44

N

EXIST. R/W

MATCH LINE STA. 729+50

730+00

735+00

740+00

MATCH LINE STA. 744+75

CANADIAN NATIONAL RAILWAY

CP19

EXIST. R/W

5

SCALE, FEET 0 50 100

5

N

EXIST. R/W

MATCH LINE STA. 744+75

EXISTING 4-64"X43"X68' LONG STEEL PLATE PIPE ARCHES
REMOVING SMALL PIPE CULVERT, 4 REQ'D
REMOVING CONCRETE HEADWALL, 2 REQ'D

CULVERT REPLACEMENT (STA. 746+93 TO 748+23)
STRUCTURE B-29-159, STA. 747+49, 747+59, 747+69
CPRCHE CLASS HE-III 43X68-INCH - 64 LF, OFF 32' LT & RT - 3 REQ'D,
APRON ENDWALLS FOR CPRCHE 43X68-INCH - 6 REQ'D,
INLET & OUTLET ELEV = 933.26' (SEE CROSS SECTIONS)

SLOPE INTERCEPT

CP20

90°00'00"

CP21

SLOPE INTERCEPT

PI: 757+81.64

745+00

750+00

755+00

760

ENVIRONMENTALLY
SENSITIVE AREA

CP22

MATCH LINE STA. 760+00

CANADIAN NATIONAL RAILWAY

EXIST. R/W

LEGEND

- CULVERT REPLACEMENT
- BUTT JOINT
- INLET PROTECTION TYPE C
- TEMPORARY DITCH CHECK
- CULVERT PIPE CHECK
- FLOW ARROW
- SILT FENCE
- EROSION MAT URBAN CLASS I TYPE A

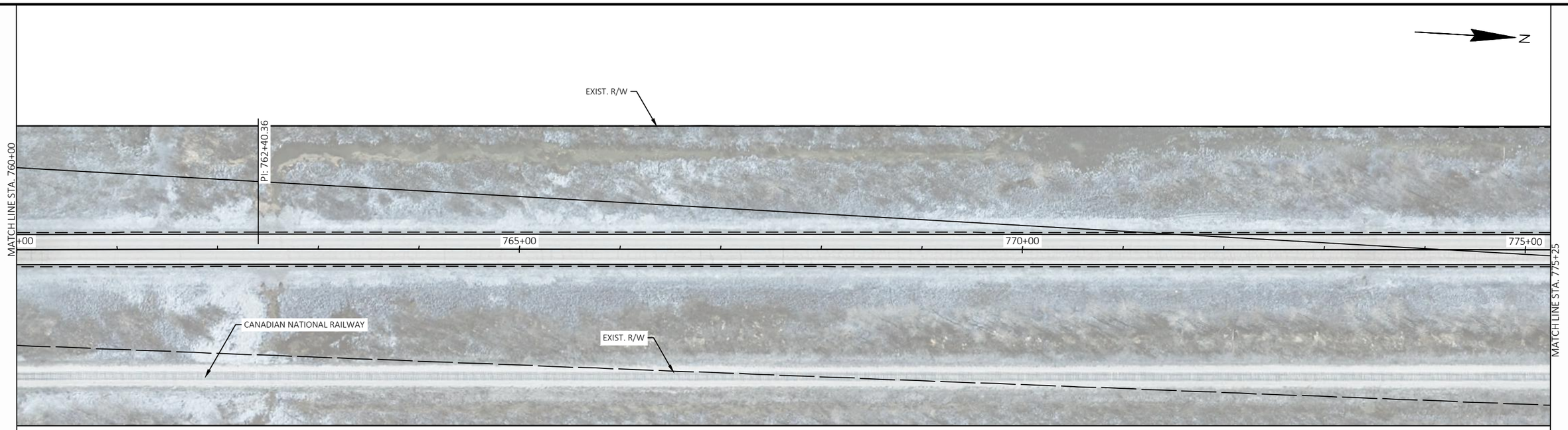
SCALE, FEET 0 50 100

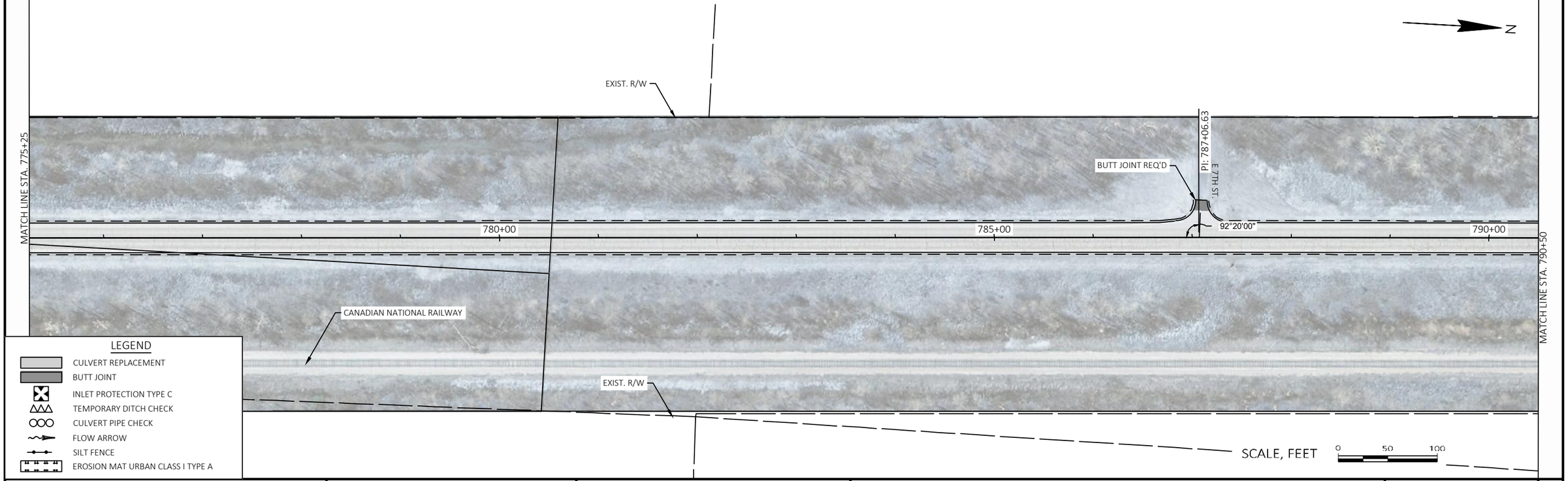
PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	PLAN	SHEET 70	E

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\050201_PN.DWG
LAYOUT NAME - Sheet - 20

PLOT DATE : 9/21/2022 2:38 PM PLOT BY : MARZOLF, ROBBI PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44



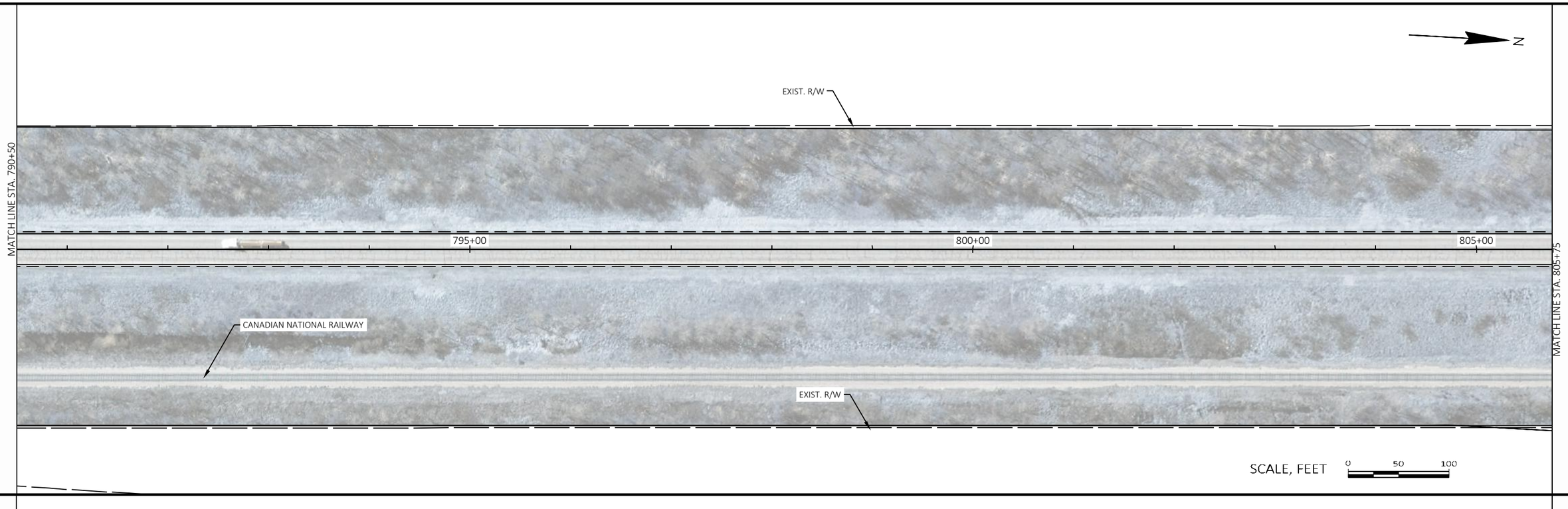


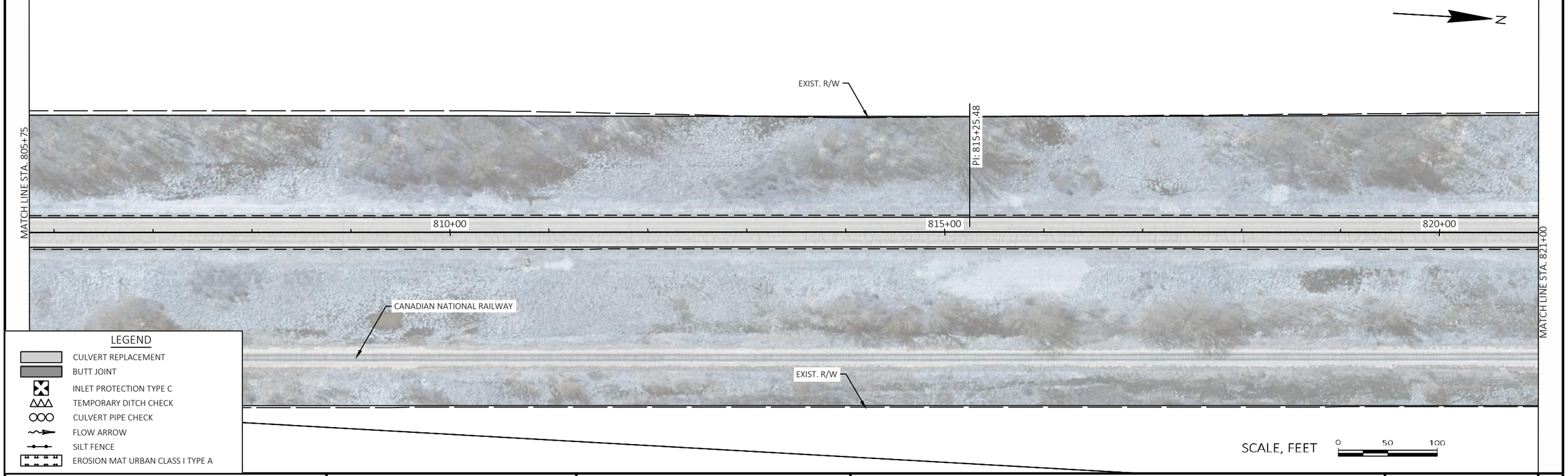
PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	PLAN	SHEET 71	E

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\050201_PN.DWG
LAYOUT NAME - Sheet - 21

PLOT DATE : 9/21/2022 2:39 PM PLOT BY : MARZOLF, ROBBI PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44





LEGEND

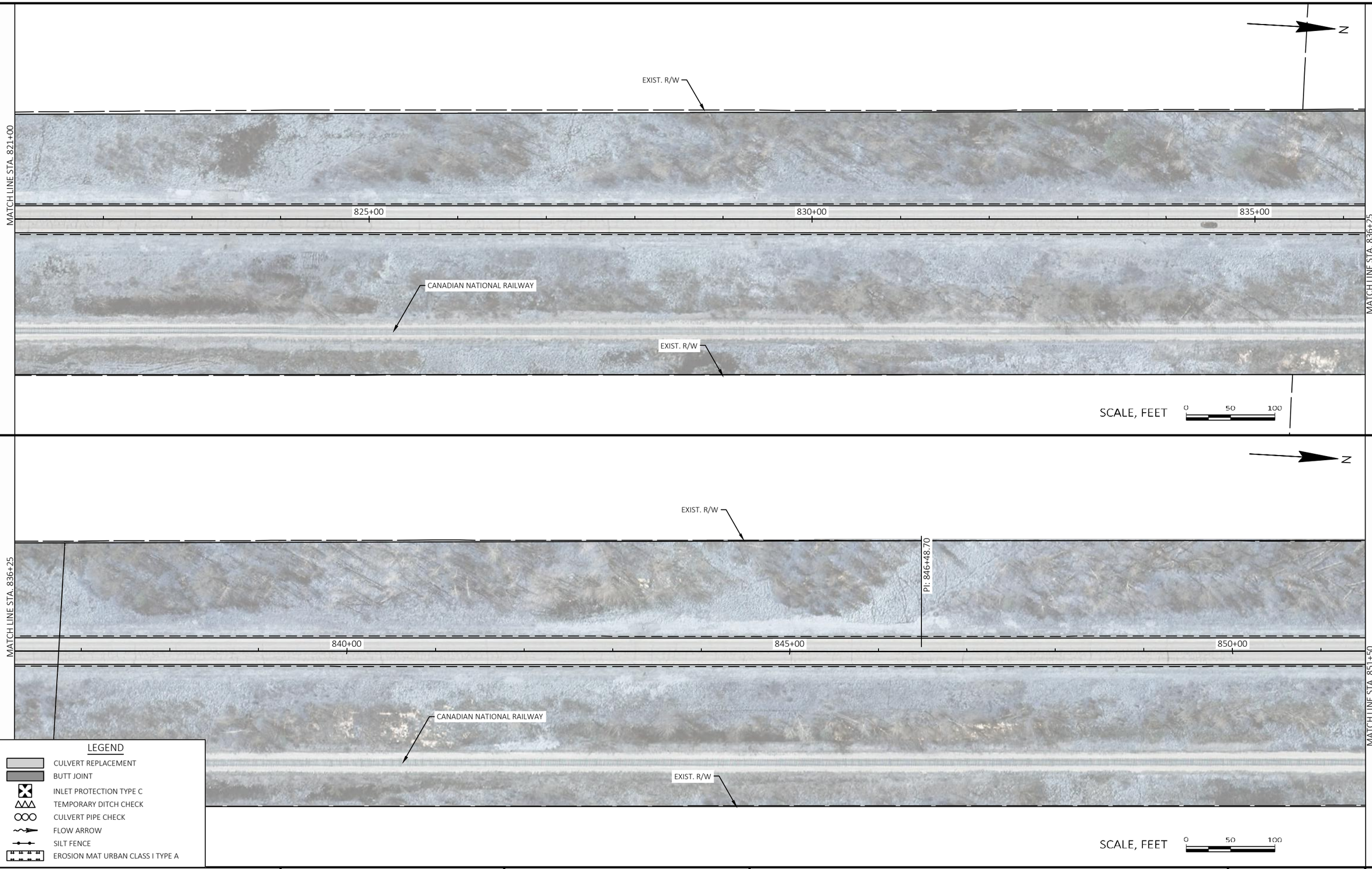
- CULVERT REPLACEMENT
- BUTT JOINT
- INLET PROTECTION TYPE C
- TEMPORARY DITCH CHECK
- CULVERT PIPE CHECK
- FLOW ARROW
- SILT FENCE
- EROSION MAT URBAN CLASS I TYPE A

PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	PLAN	SHEET 72

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\050201_PN.DWG
LAYOUT NAME - Sheet - 22

PLOT DATE : 9/21/2022 2:39 PM PLOT BY : MARZOLF, ROBBI PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44



LEGEND

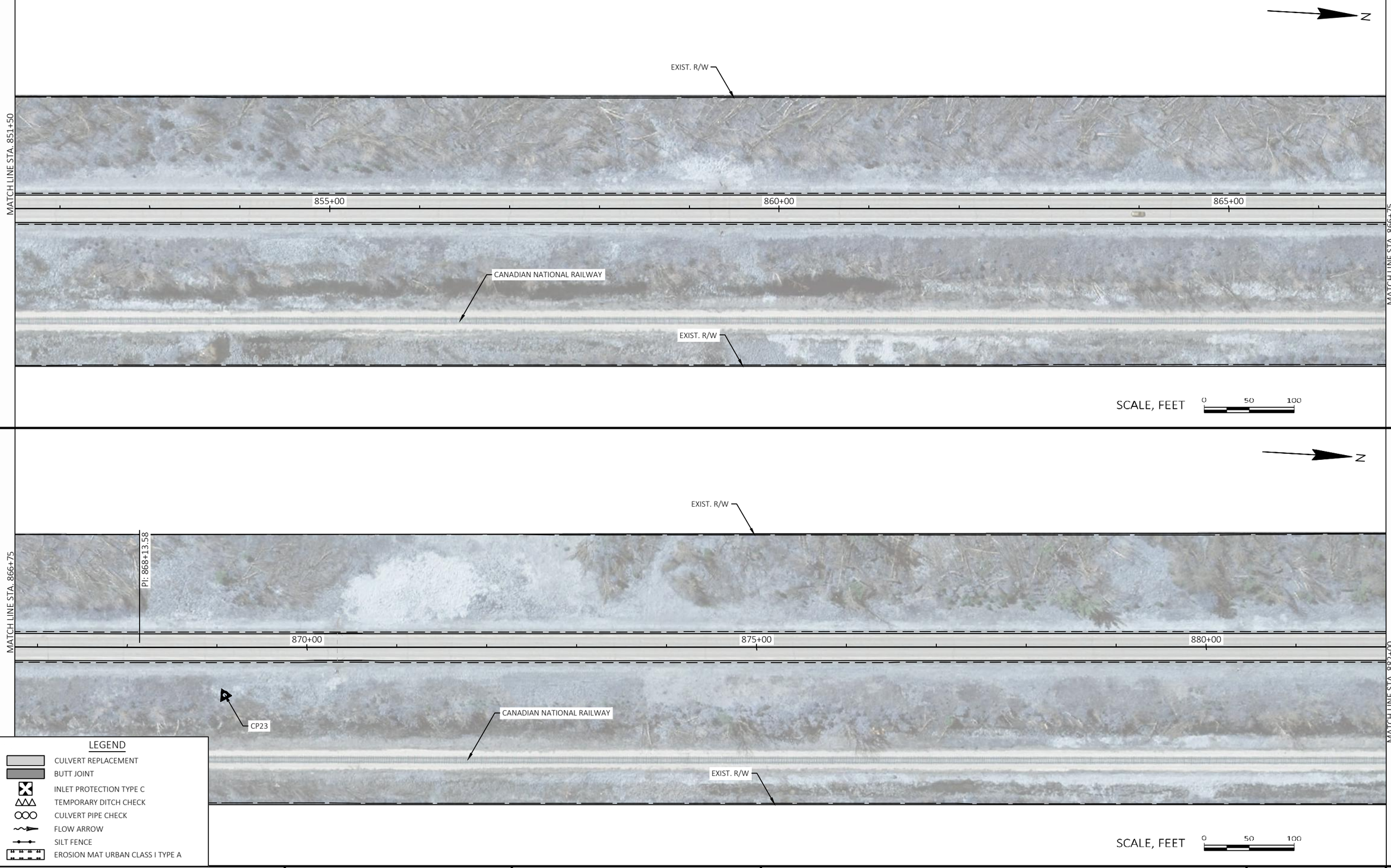
- CULVERT REPLACEMENT
- BUTT JOINT
- INLET PROTECTION TYPE C
- TEMPORARY DITCH CHECK
- CULVERT PIPE CHECK
- FLOW ARROW
- SILT FENCE
- EROSION MAT URBAN CLASS I TYPE A

PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	PLAN	SHEET 73	E

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\050201_PN.DWG
LAYOUT NAME - Sheet - 23

PLOT DATE : 9/21/2022 2:39 PM PLOT BY : MARZOLF, ROBBI PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44



5

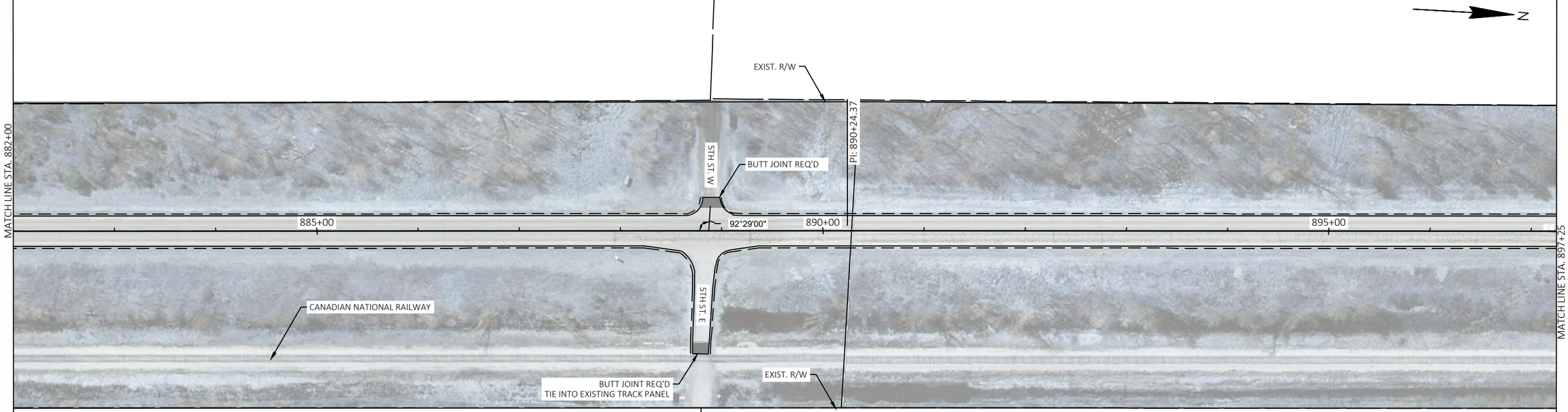
5

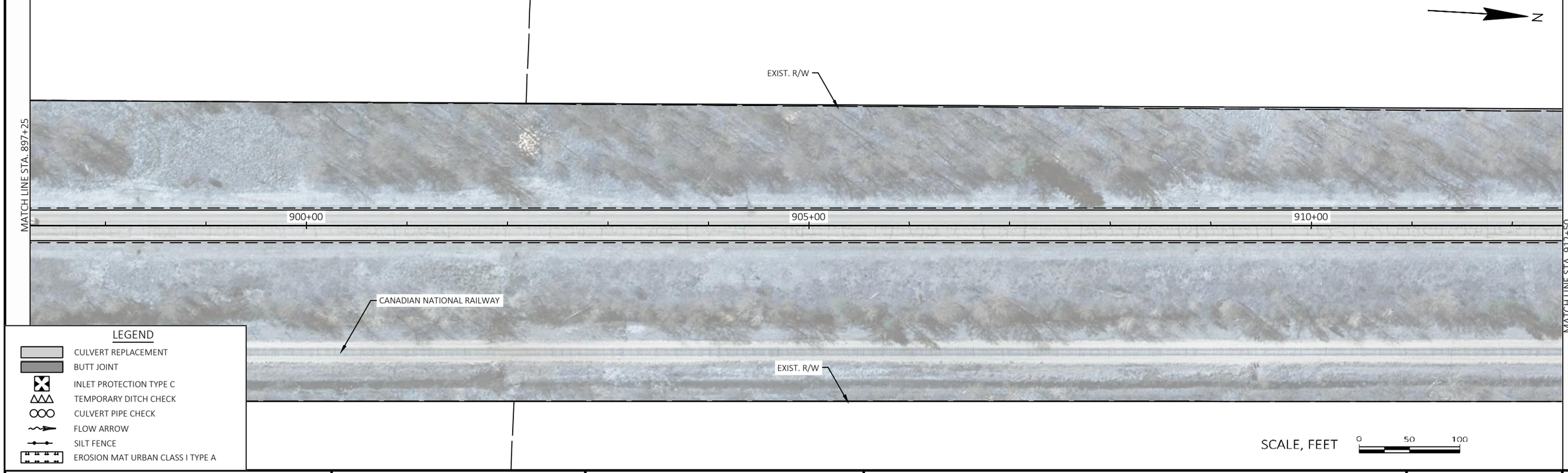
PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	PLAN	SHEET 74	E

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\050201_PN.DWG
LAYOUT NAME - Sheet - 24

PLOT DATE : 9/21/2022 2:39 PM PLOT BY : MARZOLF, ROBBI PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44





LEGEND

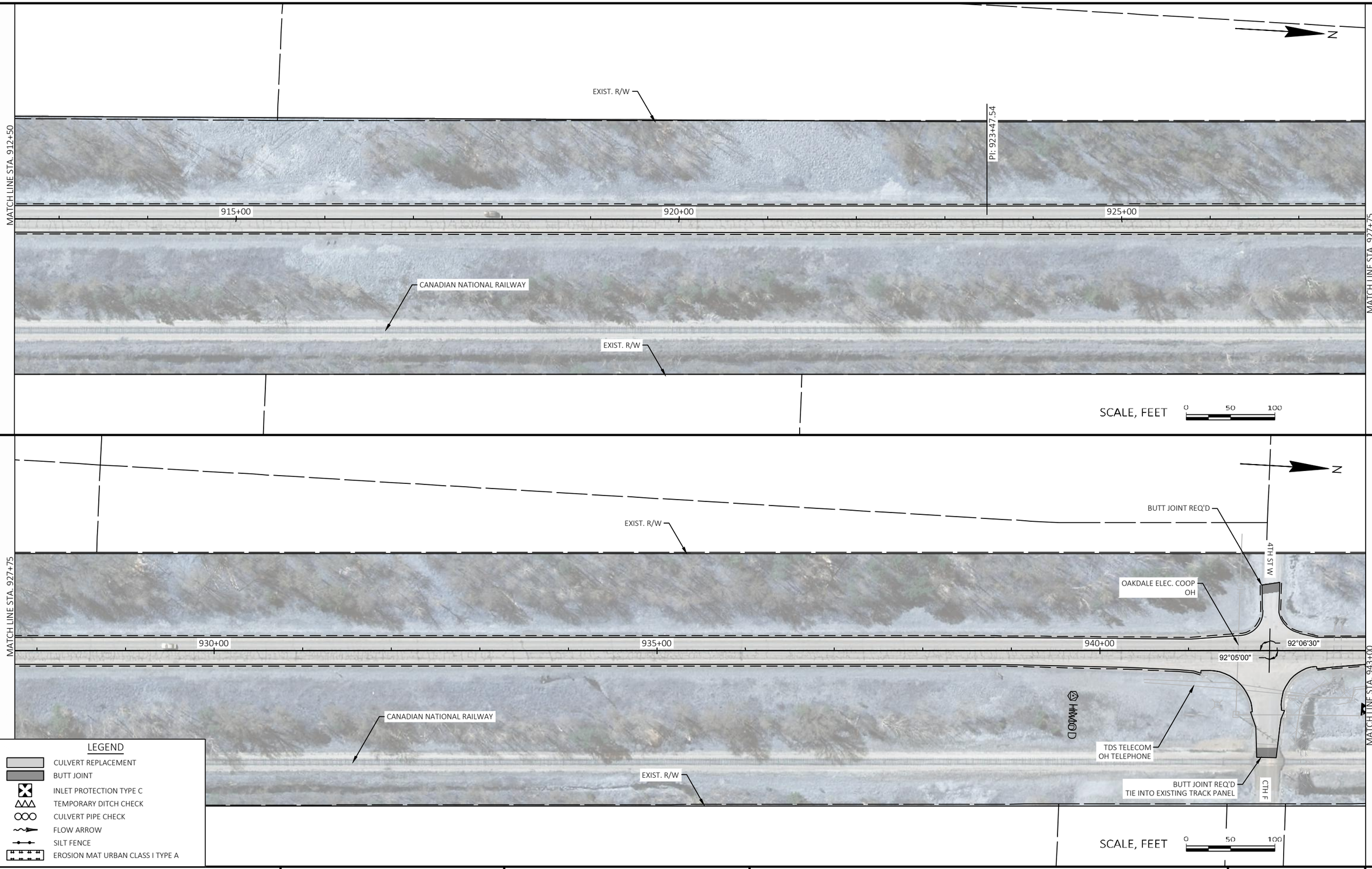
- CULVERT REPLACEMENT
- BUTT JOINT
- INLET PROTECTION TYPE C
- TEMPORARY DITCH CHECK
- CULVERT PIPE CHECK
- FLOW ARROW
- SILT FENCE
- EROSION MAT URBAN CLASS I TYPE A

PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	PLAN	SHEET 75	E

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\050201_PN.DWG
LAYOUT NAME - Sheet - 25

PLOT DATE : 9/21/2022 2:39 PM PLOT BY : MARZOLF, ROBBI PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44



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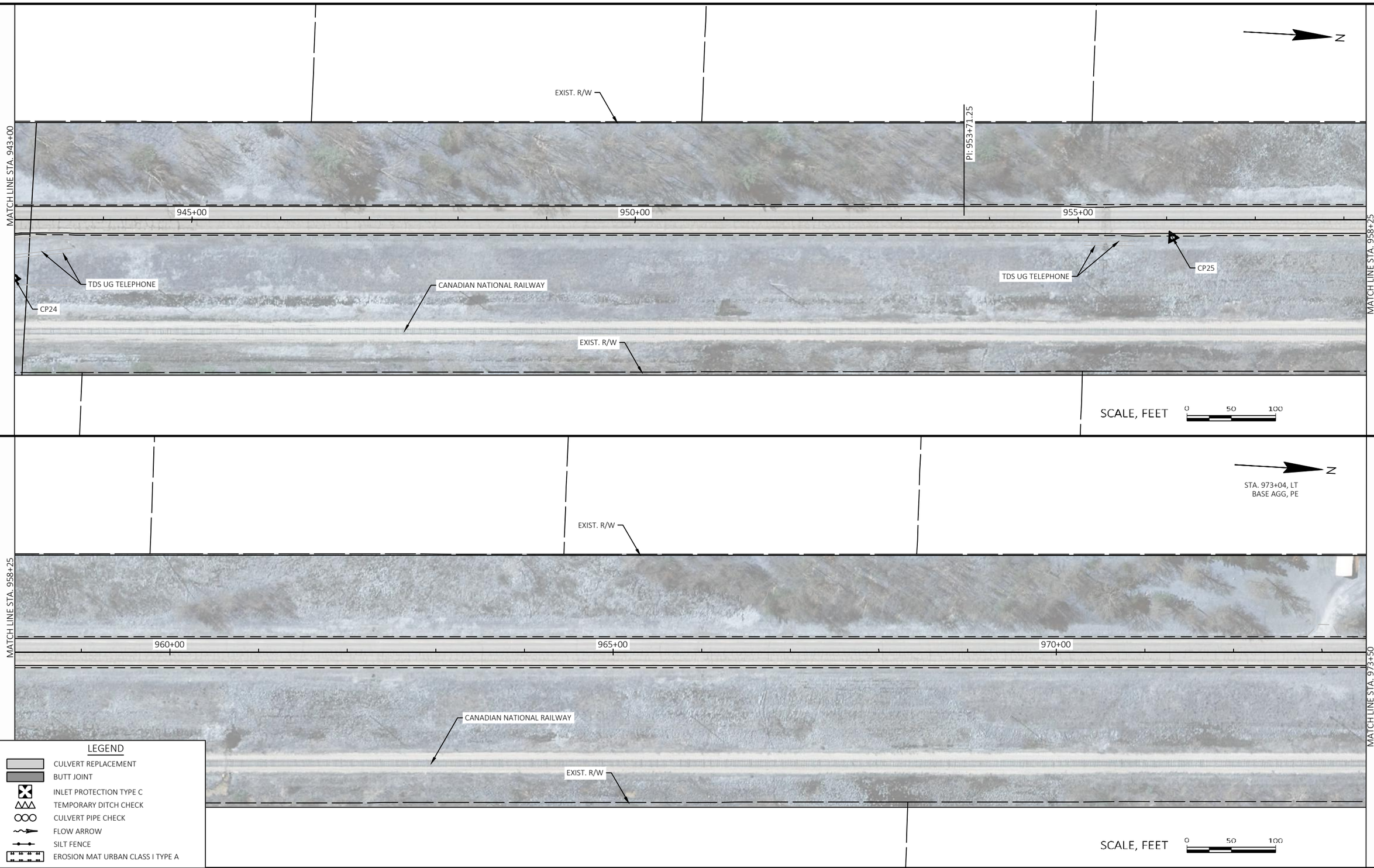
5

PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	PLAN	SHEET 76	E

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\050201_PN.DWG
LAYOUT NAME - Sheet - 26

PLOT DATE : 9/21/2022 2:40 PM PLOT BY : MARZOLF, ROBBI PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44



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5

PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	PLAN	SHEET 77	E

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\050201_PN.DWG
LAYOUT NAME - Sheet - 27

PLOT DATE : 9/21/2022 2:40 PM PLOT BY : MARZOLF, ROBBI PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44

STA. 974+84, LT
BASE AGG, PE

N

EXIST. R/W

PI: 981+79.92

MATCH LINE STA. 973+50

975+00

980+00

985+00

MATCH LINE STA. 988+75

LUBECK TRL.

CANADIAN NATIONAL RAILWAY

EXIST. R/W

STA. 975+04, RT
BASE AGG, PE

SCALE, FEET 0 50 100

5

5

N

EXIST. R/W

MATCH LINE STA. 988+75

3RD ST.

BUTT JOINT REQ'D

990+00

995+00

1000+00

88°27'15"

LUBECK TRL.

3RD ST.

CANADIAN NATIONAL RAILWAY

BUTT JOINT REQ'D
TIE INTO EXISTING TRACK PANEL

EXIST. R/W

MATCH LINE STA. 1004+00

SCALE, FEET 0 50 100

LEGEND

- CULVERT REPLACEMENT
- BUTT JOINT
- INLET PROTECTION TYPE C
- TEMPORARY DITCH CHECK
- CULVERT PIPE CHECK
- FLOW ARROW
- SILT FENCE
- EROSION MAT URBAN CLASS I TYPE A

PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	PLAN	SHEET 78	E

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\050201_PN.DWG
LAYOUT NAME - Sheet - 28

PLOT DATE : 9/21/2022 2:40 PM PLOT BY : MARZOLF, ROBBI PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44

STA. 1015+40, LT
BASE AGG, PE

N

EXIST. R/W

PI: 1015+96.64

MATCH LINE STA. 1004+00

1005+00

1010+00

1015+00

MATCH LINE STA. 1019+25

CANADIAN NATIONAL RAILWAY

EXIST. R/W

SCALE, FEET 0 50 100

5

5

N

EXIST. R/W

MATCH LINE STA. 1019+25

1020+00

1025+00

1030+00

MATCH LINE STA. 1034+50

CANADIAN NATIONAL RAILWAY

EXIST. R/W

SCALE, FEET 0 50 100

LEGEND

- CULVERT REPLACEMENT
- BUTT JOINT
- INLET PROTECTION TYPE C
- TEMPORARY DITCH CHECK
- CULVERT PIPE CHECK
- FLOW ARROW
- SILT FENCE
- EROSION MAT URBAN CLASS I TYPE A

PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	PLAN	SHEET 79	E

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\050201_PN.DWG
LAYOUT NAME - Sheet - 29

PLOT DATE : 9/21/2022 2:40 PM PLOT BY : MARZOLF, ROBBI PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44

STA. 1034+94, LT
BASE AGG, PE

N

EXIST. R/W

PI: 1045+61.49

BUTT JOINT REQ'D

2ND ST.

ENVIRONMENTALLY SENSITIVE AREA

MATCH LINE STA. 1034+50

1035+00

1040+00

1045+00

91°11'45"

OAKDALE ELEC. COOP OH

MATCH LINE STA. 1049+75

CANADIAN NATIONAL RAILWAY

EXIST. R/W

BUTT JOINT REQ'D
TIE INTO EXISTING TRACK PANEL

2ND ST.

SCALE, FEET 0 50 100

5

5

STA. 1061+84, LT
BASE AGG, PE

N

EXIST. R/W

MATCH LINE STA. 1049+75

1050+00

1055+00

1060+00

1065

MATCH LINE STA. 1065+00

CANADIAN NATIONAL RAILWAY

EXIST. R/W

LEGEND

- CULVERT REPLACEMENT
- BUTT JOINT
- INLET PROTECTION TYPE C
- TEMPORARY DITCH CHECK
- CULVERT PIPE CHECK
- FLOW ARROW
- SILT FENCE
- EROSION MAT URBAN CLASS I TYPE A

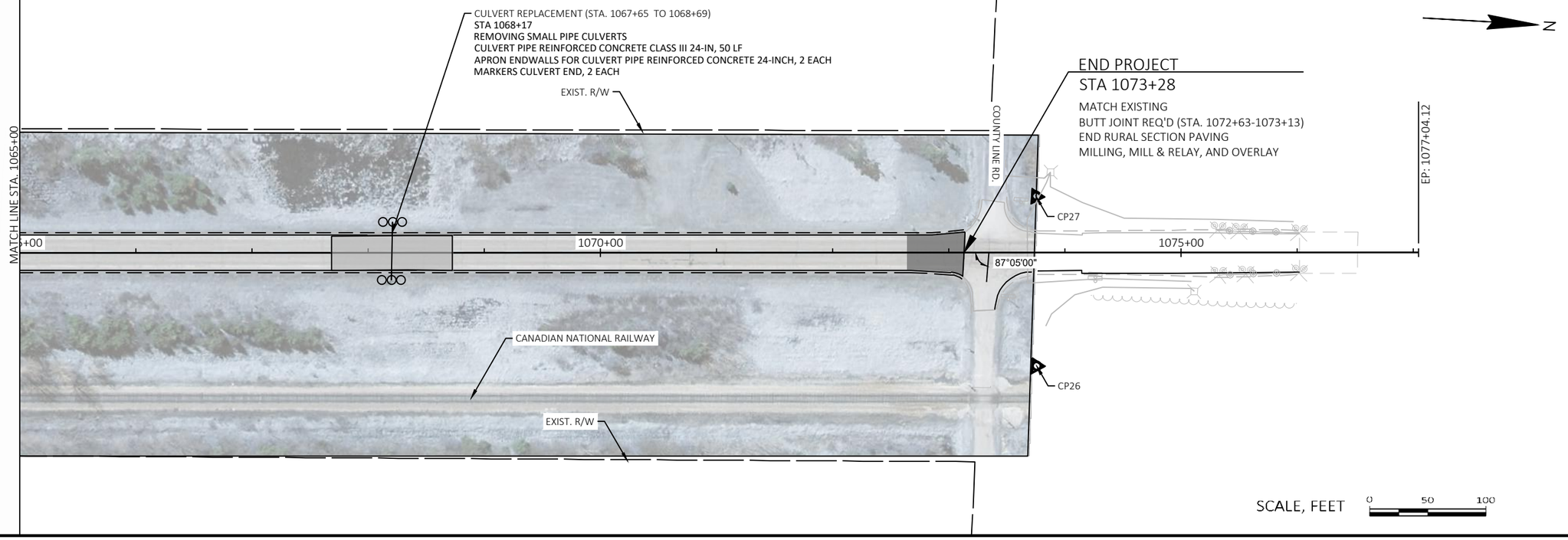
SCALE, FEET 0 50 100

PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	PLAN	SHEET 80	E

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\050201_PN.DWG
LAYOUT NAME - Sheet - 30

PLOT DATE : 9/21/2022 2:40 PM PLOT BY : MARZOLF, ROBBI PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44



5

5

LEGEND

- CULVERT REPLACEMENT
- BUTT JOINT
- INLET PROTECTION TYPE C
- TEMPORARY DITCH CHECK
- CULVERT PIPE CHECK
- FLOW ARROW
- SILT FENCE
- EROSION MAT URBAN CLASS I TYPE A

PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	PLAN	SHEET 81	E

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\050201_PN.DWG
LAYOUT NAME - Sheet - 31

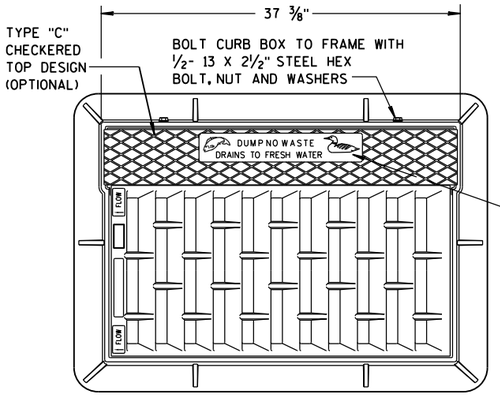
PLOT DATE : 9/21/2022 2:40 PM PLOT BY : MARZOLF, ROBBI PLOT NAME : PLOT SCALE : 1 IN:100 FT

WISDOT/CADDS SHEET 44

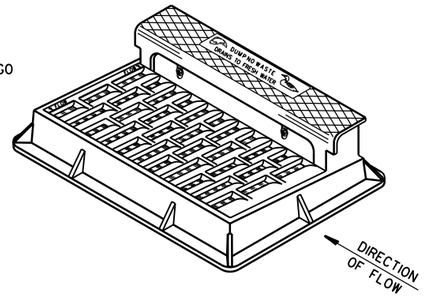
Standard Detail Drawing List

08A05-19A	INLET COVERS TYPE A, H, A-S, H-S & Z
08D01-22A	CONCRETE CURB & GUTTER
08D01-22B	CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS
08D05-20A	CURB RAMPS TYPES 1 AND 1-A
08D05-20B	CURB RAMPS TYPES 2 AND 3
08D05-20D	CURB RAMPS TYPE 4B AND 4B1
08D05-20E	CURB RAMPS TYPES 5, 6, 7A, 7B & 8
08D05-20F	CURB RAMPS RADIAL DETECTABLE WARNING FIELD APPLICATIONS
08D05-20G	CURB RAMPS RECTANGULAR AND RADIAL DETECTABLE WARNING PLATES
08D22-01	DRIVEWAYS WITHOUT CURB & GUTTER RESURFACING PROJECTS RURAL
08E08-03	TYPICAL INSTALLATIONS OF EROSION BALES / TEMPORARY DITCH CHECKS
08E09-06	SILT FENCE
08E10-02	INLET PROTECTION TYPE A, B, C AND D
08E15-01	CULVERT PIPE CHECK
08F02-01	APRON ENDWALLS FOR PIPE ARCH AND ELLIPTICAL PIPE
08F04-08	JOINT TIES FOR CONCRETE PIPE AND CONCRETE COLLAR DETAIL
13A11-03A	2-LANE RURAL CENTER LINE RUMBLE STRIP, MILLING
13A11-03B	2-LANE RURAL CENTER LINE RUMBLE STRIP, MILLING
13B01-10	PAVEMENT DETAILS FOR RAILROAD APPROACH
13C19-03	HMA LONGITUDINAL JOINTS
14B29-01	SAFETY EDGE
15A03-02A	FLEXIBLE MARKER POST FOR CULVERT END
15A03-02B	FLEXIBLE MARKER POST FOR CULVERT END
15C02-08A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-08C	DETOUR SIGNING FOR MAINLINE CLOSURES
15C03-05	BARRICADES AND SIGNS FOR SIDEROAD CLOSURES
15C04-05	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M.P.H. OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC
15C05-05	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M.P.H. OR LESS
15C08-21A	LONGITUDINAL MARKING (MAINLINE)
15C08-21B	TEMPORARY LONGITUDINAL PAVEMENT MARKING
15C08-21C	PAVEMENT MARKING (TURN LANES)
15C08-21D	PAVEMENT MARKING (TURN LANES)
15C11-09B	CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS
15C12-09A	TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION
15C19-07A	MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY
15C33-04	STOP LINE AND CROSSWALK PAVEMENT MARKING
15C35-05A	PAVEMENT MARKING (INTERSECTIONS)
15D28-04	TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY
15D30-07A	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-07B	TRAFFIC CONTROL, TEMPORARY ADA COMPLIANT PEDESTRIAN ACCOMMODATION
15D30-07C	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-07D	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-07E	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-07F	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-07G	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-07H	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-07I	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-07J	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION

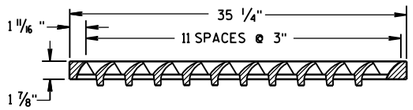
6

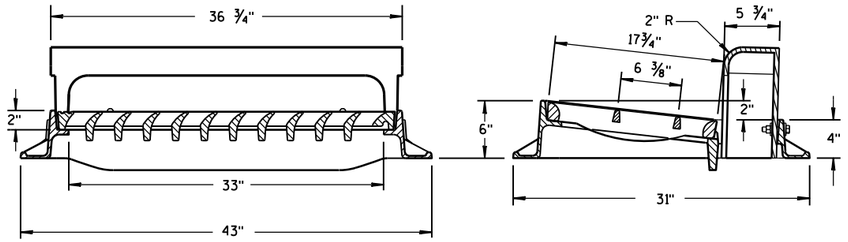


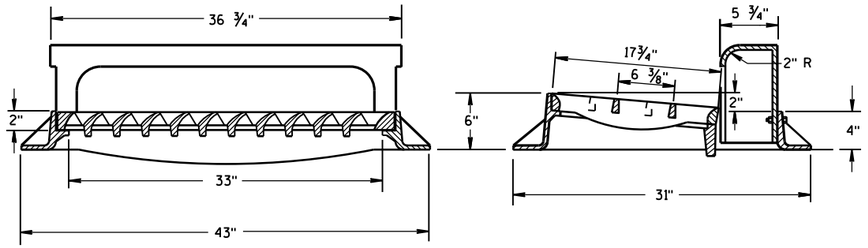
NOTE:
GRATE IS REVERSIBLE.



NOTE: CURB BOX HEIGHT ADJUSTABLE 6" TO 9"

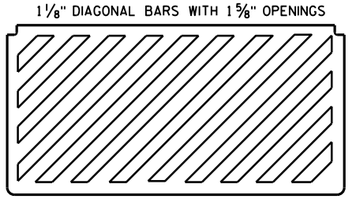






TYPE "H"

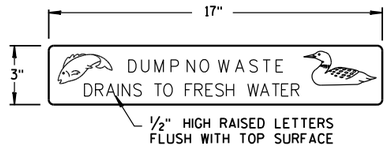
NOTE: EITHER CASTING IS ACCEPTABLE



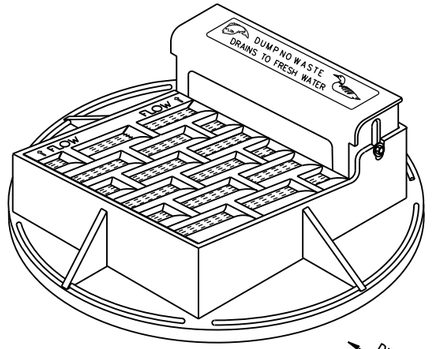
SPECIAL GRATE FOR TYPE "H" COVER

(MEASURES 35 1/4" X 17 3/4" X 2")

(NOTED AS TYPE H-S ON DRAINAGE TABLE)



LOGO DETAIL



NOTE: CURB BOX ADJUSTABLE 4" TO 9"

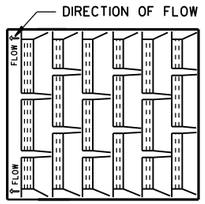
NOTE:
GRATE IS REVERSIBLE.

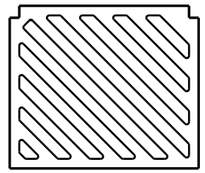
GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

DETAIL DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR CATCH BASIN, MANHOLE AND INLET COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ROUND FRAMES AND COVERS SHALL HAVE CONTINUOUSLY MACHINED BEARING SURFACES TO PREVENT ROCKING AND RATTLING.

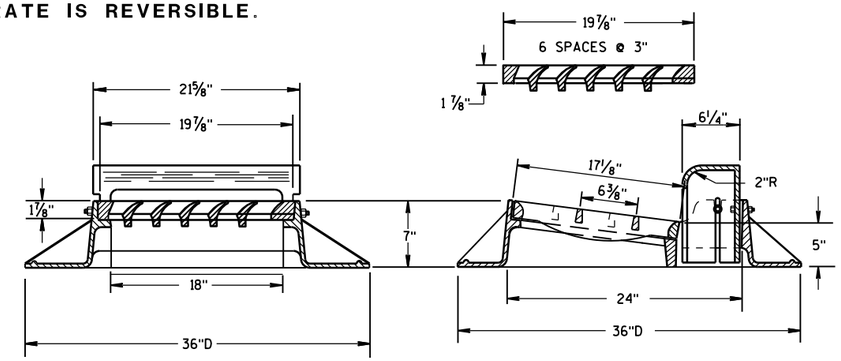




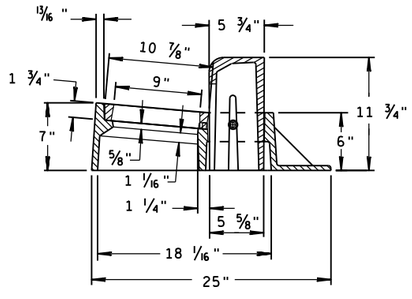
SPECIAL GRATE FOR TYPE "A" COVER

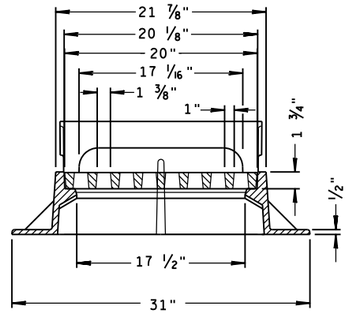
(MEASURES 19 3/4" X 17" X 1 7/8")

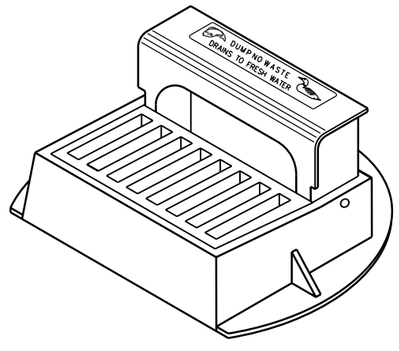
(NOTED AS TYPE A-S ON DRAINAGE TABLE)



TYPE "A"







TYPE "Z"

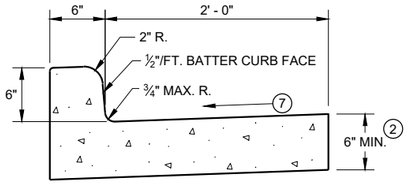
INLET COVERS
TYPE A, H, A-S, H-S & Z

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

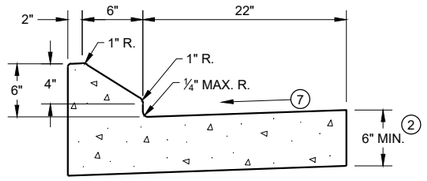
APPROVED
11-27-13 — DATE
/S/ Jerry H. Zogg — ROADWAY STANDARDS ENGINEER
FHWA

6

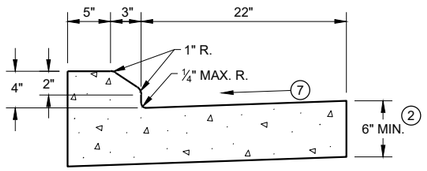
S.D.D. 8 A 5-19a



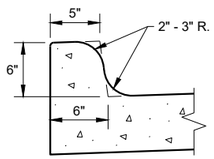
TYPES A[1] & D



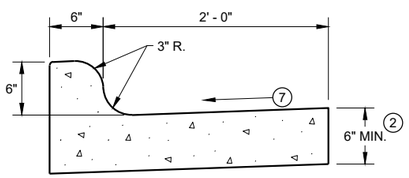
6" SLOPED CURB TYPES G[1] & J



4" SLOPED CURB TYPES G[1] & J

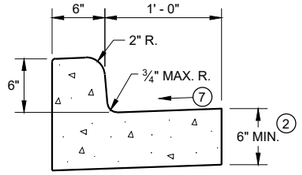


TYPES K[1] & L
(OPTIONAL CURB SHAPE)



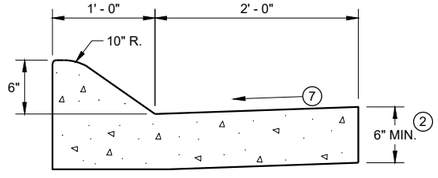
TYPES K[1] & L

CONCRETE CURB AND GUTTER 30"



TYPES A[1] & D

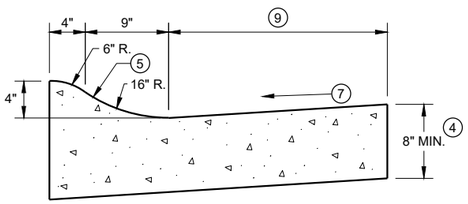
CONCRETE CURB AND GUTTER 18"



6" SLOPED CURB TYPES A[1] & D

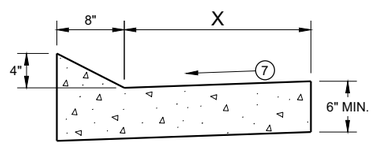
4" SLOPED CURB TYPES A[1] & D

CONCRETE CURB AND GUTTER 36"



4" SLOPED CURB TYPES R[1] & T

TBT & TBTT	X
30"	22"
36"	28"

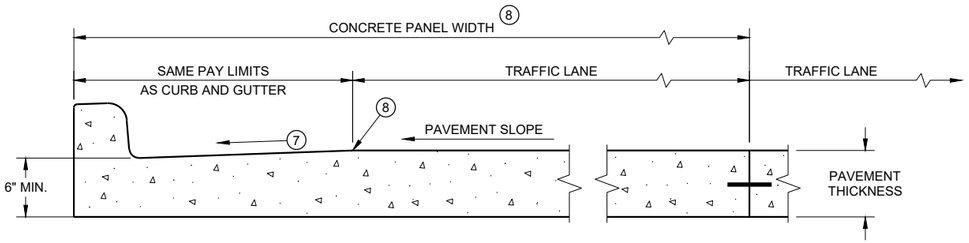


TYPES TBT & TBTT[1]

CONCRETE CURB AND GUTTER

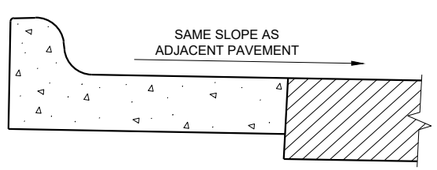
PAVEMENT THICKNESS AND MAXIMUM CONCRETE PANEL WIDTH TABLE

PAVEMENT THICKNESS	MAXIMUM PANEL WIDTH
LESS THAN 10"	12'
10" & ABOVE	15'



PARTIAL SECTION OF PAVEMENT * WITH INTEGRAL CURB AND GUTTER

* BIKE LANE IS NOT SHOWN



REVERSE SLOPE GUTTER[6]
(TYPICAL FOR ALL CURB & GUTTER TYPES)

GENERAL NOTES

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

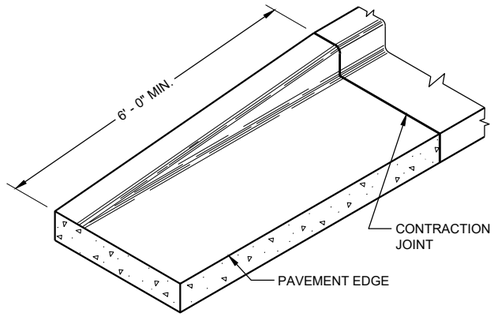
PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

INTEGRAL CURB AND GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB AND GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE.

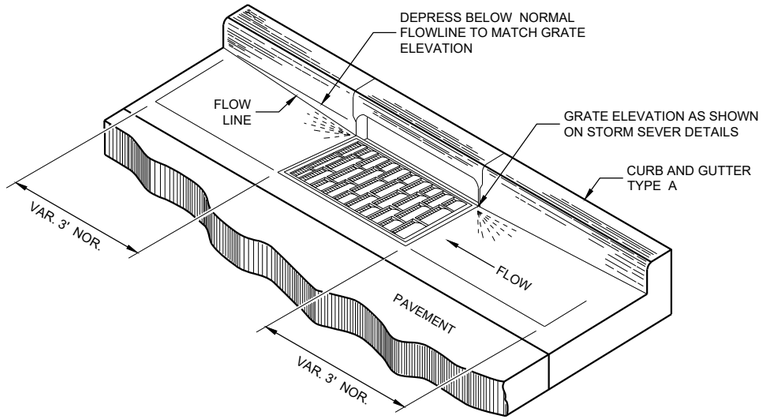
UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

1. TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
2. THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
3. USE 8" MINIMUM GUTTER THICKNESS WHEN USED WITH AN ADJACENT CONCRETE TRUCK APRON PLACED BEHIND BACK OF CURB.
4. THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.
5. UNLESS OTHERWISE NOTED, FOR STAKING PURPOSES THE FACE OF CURB IS 6" FROM THE BACK OF CURB.
6. WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.
7. USE 4% GUTTER CROSS SLOPE UNLESS OTHERWISE NOTED IN THE PLANS.
8. INCLUDE LONGITUDINAL JOINT AND TIE BARS ALONG LANE EDGE WHEN CONCRETE PANEL WIDTH EXCEEDS THE MAXIMUM WIDTH PER TABLE BELOW. LONGITUDINAL JOINT(S) ARE NOT ALLOWED WITHIN TRAFFIC LANES AND BIKE LANES. LONGITUDINAL JOINT MAY BE SAWED.
9. CONCRETE CURB AND GUTTER 4-INCH SLOPED 30-INCH TYPE "R" AND "T" = 17 INCHES
   CONCRETE CURB AND GUTTER 4-INCH SLOPED 36-INCH TYPE "R" AND "T" = 23 INCHES

CONCRETE CURB AND GUTTER



END SECTIONCURB AND GUTTER



DETAIL OF CURB AND GUTTER AT INLETS
(TYPICAL H INLET COVER SHOWN)

GENERAL NOTES

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

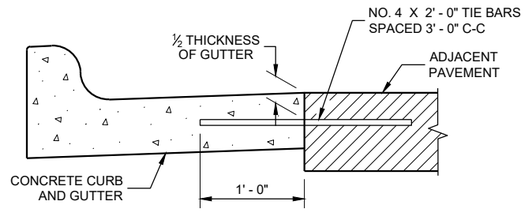
PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

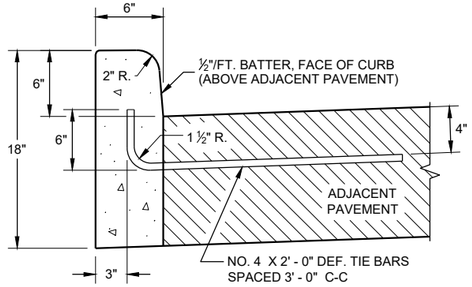
(1) TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.

(2) THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.

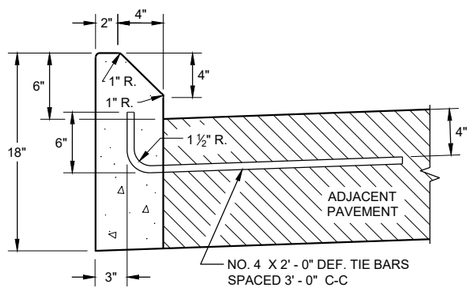
(9) REFER TO SDD 08D18 AND 08D19 FOR ADDITIONAL DRIVEWAY ENTRANCE CURB DETAILS.



TYPICAL TIE BAR LOCATION (1)

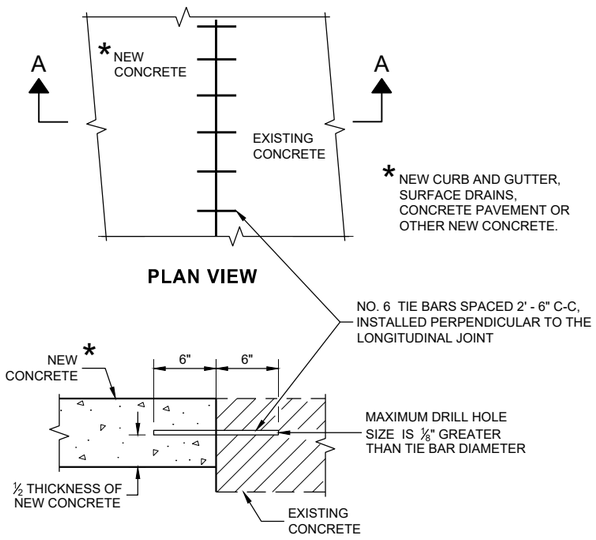


TYPES A (1) & D



TYPES G (1) & J

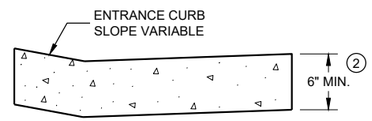
CONCRETE CURB



PLAN VIEW

SECTION A - A

TIE BARS DRILLED INTO EXISTING PAVEMENT



DRIVEWAY ENTRANCE CURB (9)
(WHEN DIRECTED BY THE ENGINEER)

CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

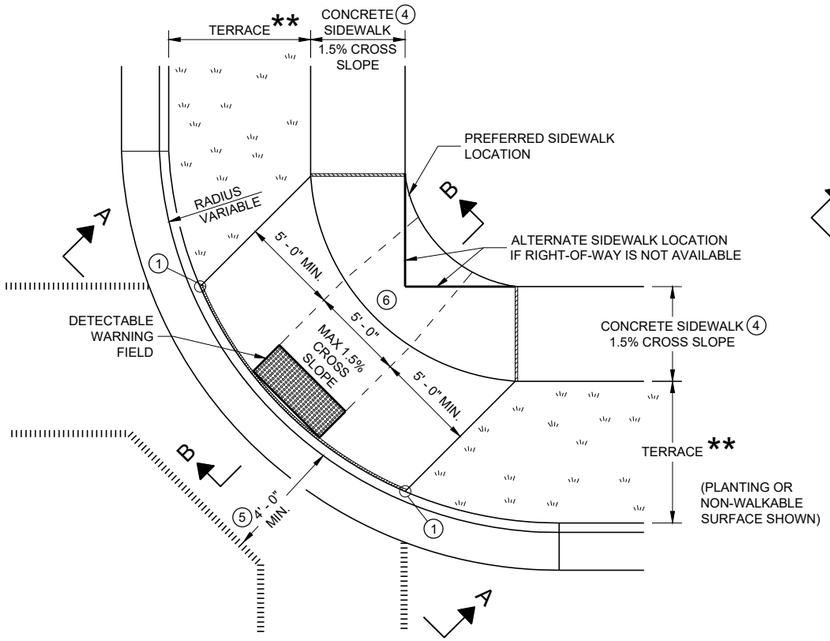
APPROVED
February 2021 — DATE
/S/ Rodnery Taylor — ROADWAY STANDARDS DEVE ENGINEER

FHWA

6

SDD08D01 - 22b

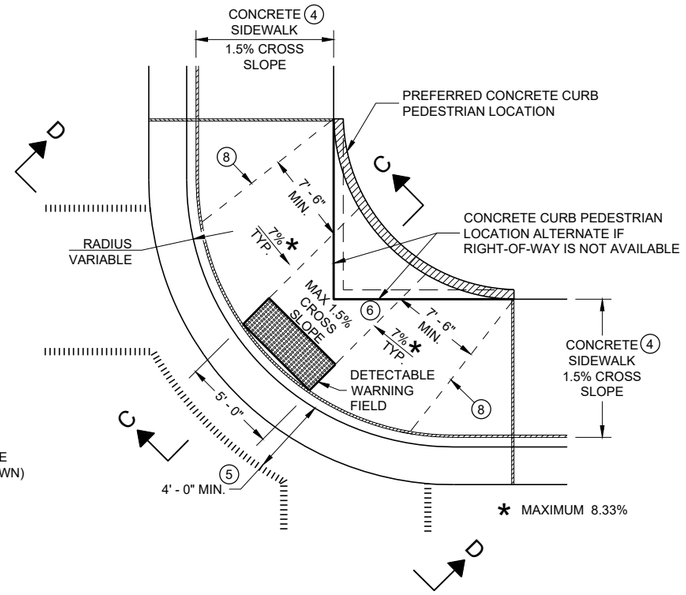
SDD08D01 - 22b



PLAN VIEW

CURB RAMP TYPE 1

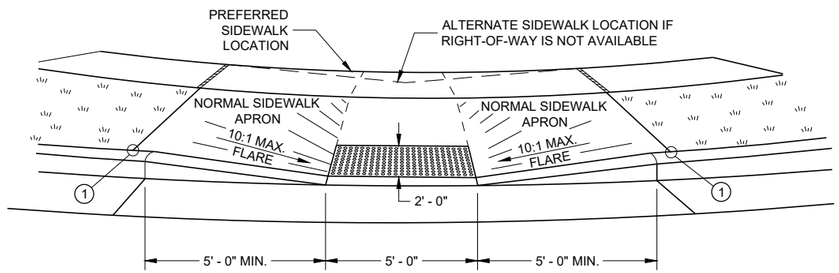
(CENTER OF CORNER RADIUS)



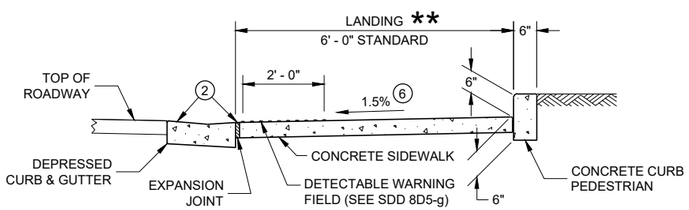
PLAN VIEW

CURB RAMP TYPE 1 - A

(NO TERRACE)

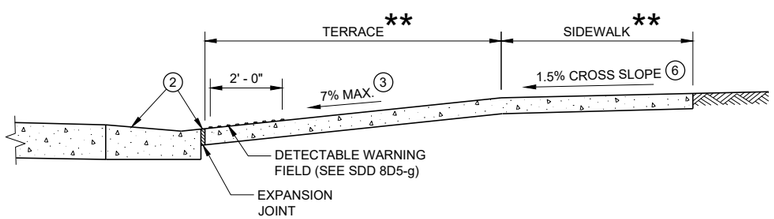


VIEW A - A FOR TYPE 1

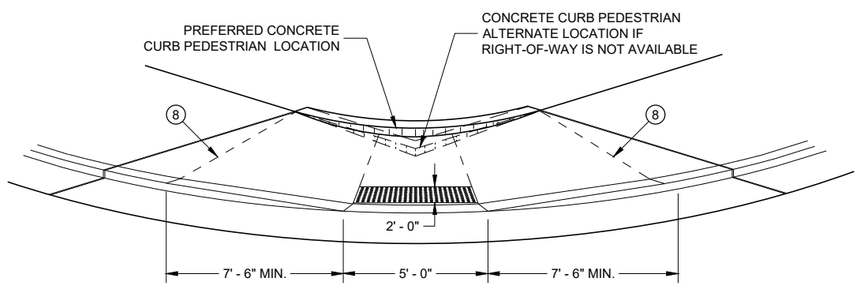


SECTION C - C FOR TYPE 1 - A

** WIDTH SHOWN ELSEWHERE IN THE PLANS



SECTION B - B FOR TYPE 1



VIEW D - D FOR TYPE 1 - A

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

WHEN NECESSARY, THE SIDEWALK ELEVATION MAY BE LOWERED TO MEET THE HIGH POINT ON THE RAMP.

TYPE 1 CURB RAMPS SHALL HAVE A NORMAL SIDEWALK APRON AND CURB ON BOTH SIDES OF RAMP.

DETECTABLE WARNING FILED SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS "CURB RAMP DETECTABLE WARNING FIELD". THE CONCRETE PEDESTRIAN CURB, IF NEEDED, SHALL BE MEASURED AND PAID BY THE LINEAR FOOT AS "CONCRETE CURB PEDESTRIAN". CONCRETE SIDEWALK IN THE CURB RAMP AREA SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS CONCRETE SIDEWALK, INCLUDING THE AREA UNDER THE DETECTABLE WARNING FIELD.

SELECT CURB RAMP DETECTABLE WARNING FIELD MATERIALS AND DEVICES FROM THE DEPARTMENT'S APPROVED MATERIALS LIST. THE COLOR OF THE DETECTABLE WARNING FIELD IS SPECIFIED ELSEWHERE AND IS INCIDENTAL TO THE BID ITEM OF "CURB RAMP DETECTABLE WARNING FIELD"

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

SURFACE TEXTURE OF THE RAMP SHALL BE OBTAINED BY COARSE BROOMING TRANSVERSE TO THE SLOPE OF THE RAMP.

(1) THIS POINT IS AN EXTENSION OF OUTSIDE EDGE OF APPROACHING SIDEWALK WHERE IT MEETS THE BACK OF CONCRETE CURB. POINT LOCATION MAY BE ADJUSTED TO ALIGN WITH BEGINNING OF FULL-HEIGHT CURB IF THIS DISTANCE IS SHORT.

(2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN ¼ - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.

(3) MAXIMUM 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.

(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

(5) PROVIDE A LEVEL LANDING IN THE STREET AND GUTTER AREA (2% MAXIMUM SLOPE IN ANY DIRECTION). WHEN THE GUTTER SLOPE EXCEEDS 2%, CONSTRUCT THE LEVEL LANDING IN THE STREET AREA.

(6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.

(8) PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

LEGEND

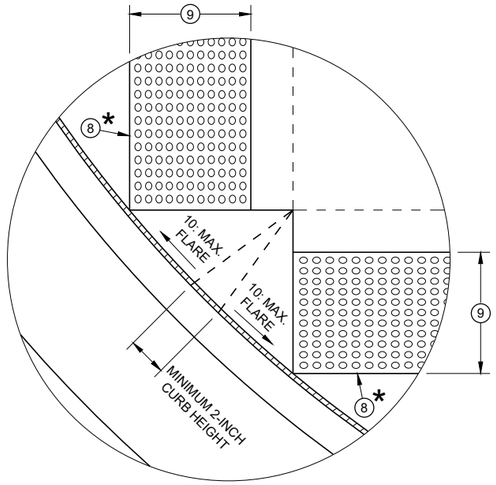
- ½" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)

CURB RAMPS
TYPE 1 AND 1-A

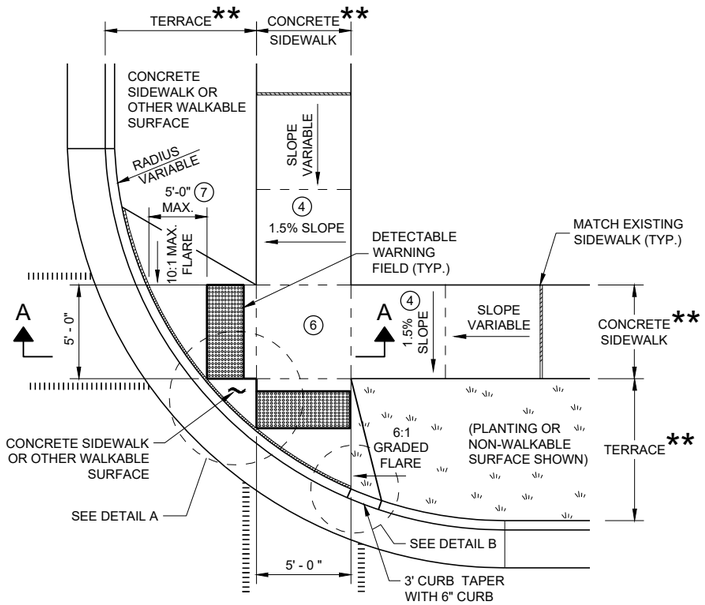
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

SDD 08D05 - 20a



DETAIL A



PLAN VIEW
CURB RAMP TYPE 2
(CENTER OF CORNER RADIUS)

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE SHALL BE FROM THE SAME MANUFACTURER.

(2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN ¼ - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.

(3) AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE (2.67% OR LESS) AND NOT TO EXCEED 11% GRADE CHANGE.

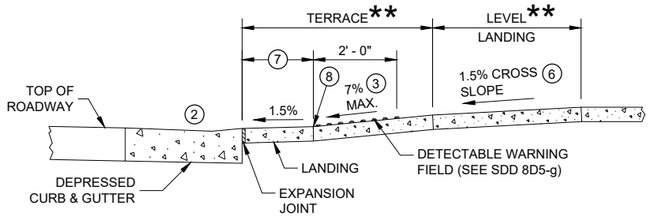
(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

(6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET X 5 FEET.

(7) WHEN GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.

(8) PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

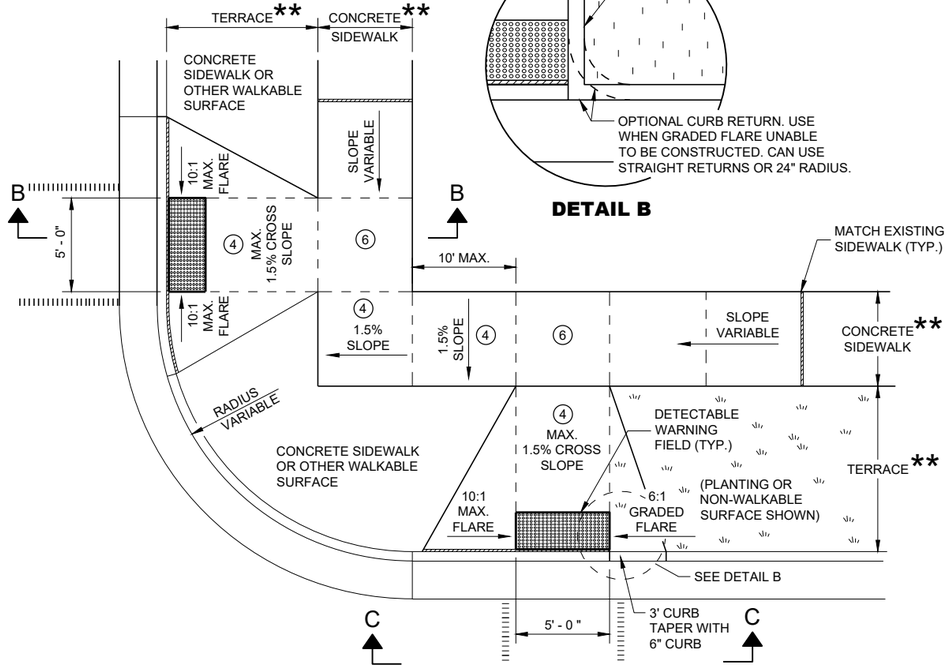
(9) WHEN DISTANCE IS LESS THAN 6' - 0", IT MAY BE DIFFICULT TO ACHIEVE A 7% SLOPE OR FLATTER ALONG THE RAMP. REDUCE CURB HEIGHT IN TRIANGLE AREA TO ACHIEVE 7% SLOPE OR FLATTER ON RAMP. CONSTRUCT 2-INCH MINIMUM CURB HEIGHT BETWEEN 10:1 FLARES.



SECTION A - A FOR TYPE 2

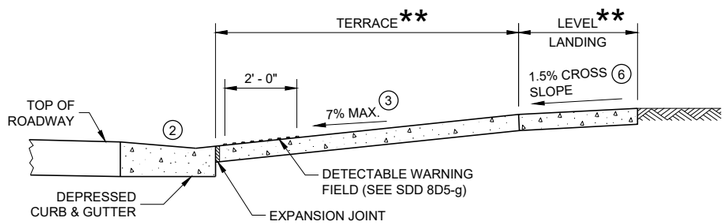
* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK

** WIDTH SHOWN ELSEWHERE IN THE PLANS

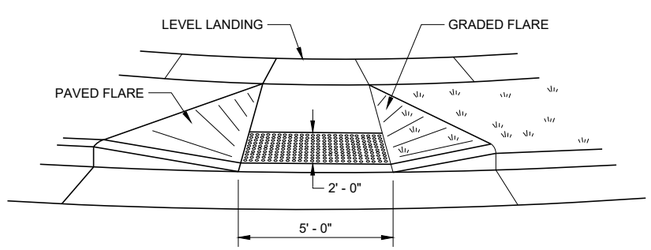


DETAIL B

PLAN VIEW
CURB RAMP TYPE 3
(OUTSIDE OF CROSSWALK AREA)



SECTION B - B FOR TYPE 3



VIEW C - C FOR TYPE 3

LEGEND

- ½" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT SIDEWALK
- PAVEMENT MARKING CROSSWALK (WHITE)

CURB RAMPS TYPE 2 AND 3

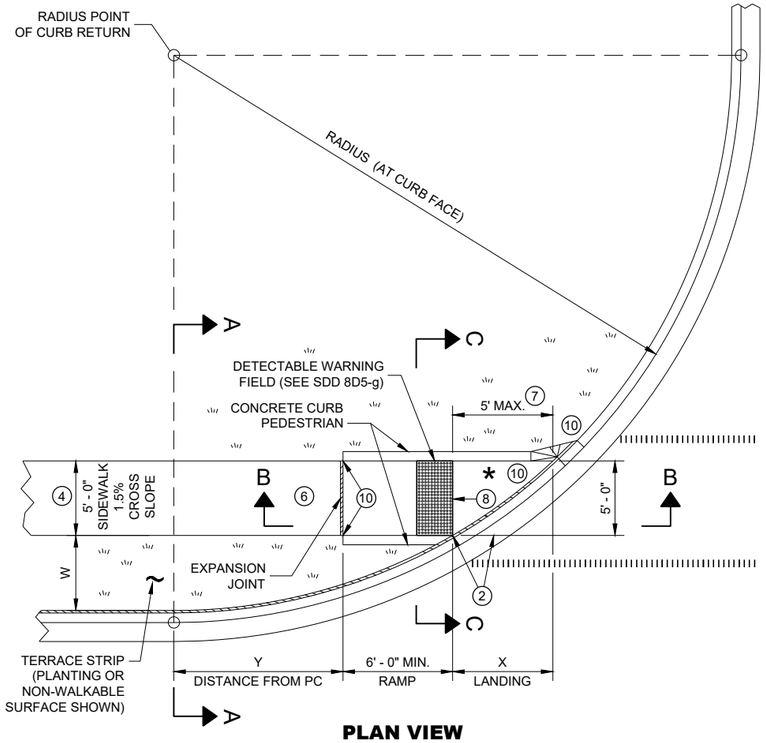
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

SDD 08D05 - 20b

6

SDD 08D05 - 20b



PLAN VIEW

CURB RAMP TYPE 4B

RADIUS (AT CURB FACE)	W = 3' - 0"		W = 4' - 0"		W = 5' - 0"		W = 6' - 0"		W = 7' - 0"		W = 8' - 0"		W = 9' - 0"		W = 10' - 0"	
	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y
10 FEET	2' - 10 ¼"	0' - 5"	2' - 1"	1' - 4 ½"	1' - 5"	2' - 1"	0' - 10"	2' - 7 ½"	0' - 3 ¼"	3' - 0 ¼"						
15 FEET	4' - 6 ¾"	2' - 1 ¾"	3' - 9"	3' - 5 ¾"	3' - 1 ¼"	4' - 6"	2' - 6 ¾"	5' - 4 ½"	2' - 1"	6' - 1"	1' - 8"	6' - 8 ½"	1' - 3 ¼"	7' - 2 ½"	0' - 10 ¾"	7' - 7 ¼"
20 FEET	5' - 9 ¾"	3' - 6 ½"	4' - 11 ½"	5' - 1 ¾"	4' - 3 ¼"	6' - 5 ½"	3' - 8 ¾"	7' - 7"	3' - 3"	8' - 6 ½"	2' - 10"	9' - 4 ½"	2' - 5 ½"	10' - 1 ¼"	2' - 1 ¼"	10' - 9"
30 FEET			6' - 9 ¼"	7' - 11 ¼"	6' - 0 ¼"	9' - 8"	5' - 5"	11' - 1 ¾"	4' - 10 ¾"	12' - 5 ¾"	4' - 5 ½"	13' - 7 ¾"	4' - 0 ¾"	14' - 8 ½"	3' - 8 ½"	15' - 8 ¼"
40 FEET									6' - 1 ¾"	15' - 8 ½"	5' - 8"	17' - 2"	5' - 3"	18' - 5 ¾"	4' - 10 ¾"	19' - 8 ¼"
50 FEET															5' - 10 ¼"	23' - 2"

INTERMEDIATE RADII CAN BE INTERPOLATED
DIMENSION "Y" IS CALCULATED BASED ON 6'-0" RAMP LENGTH
DIMENSION "X" IS CALCULATED BASED ON 5'-0" SIDEWALK WIDTH

SECTION C - C FOR TYPE 4B

LEGEND

- ½" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT SIDEWALK
- PAVEMENT MARKING CROSSWALK (WHITE)

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

(2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN ¼ - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.

(3) AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.

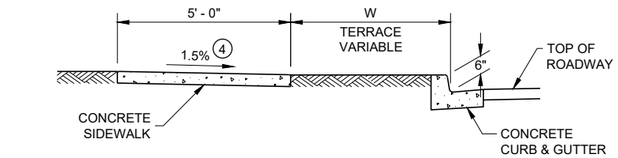
(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

(6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.

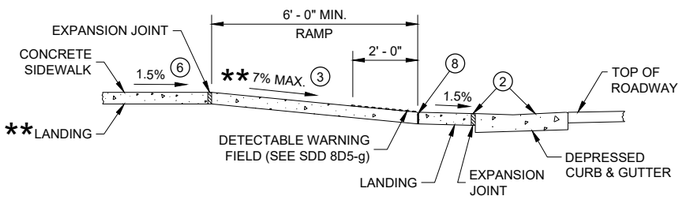
(7) WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.

(8) PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

(10) INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.



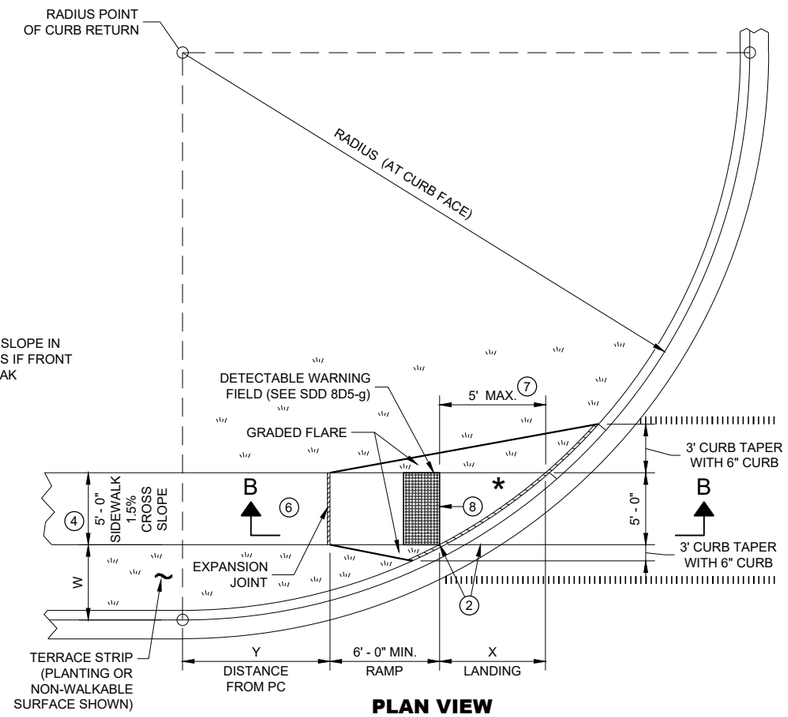
SECTION A - A FOR TYPE 4B



SECTION B - B FOR TYPE 4B AND TYPE 4B1

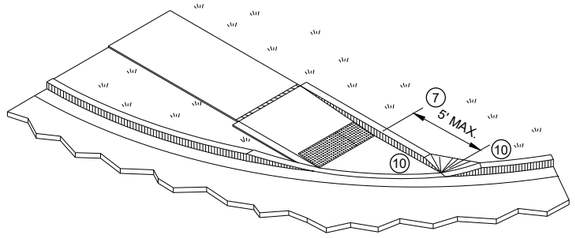
** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IF FRONT OF GRADE BREAK

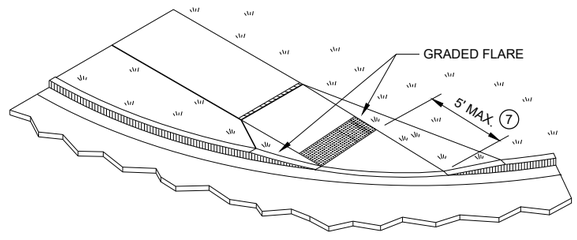


PLAN VIEW

CURB RAMP TYPE 4B1



ISOMETRIC VIEW FOR TYPE 4B



ISOMETRIC VIEW FOR TYPE 4B1

CURB RAMPS TYPE 4B AND 4B1

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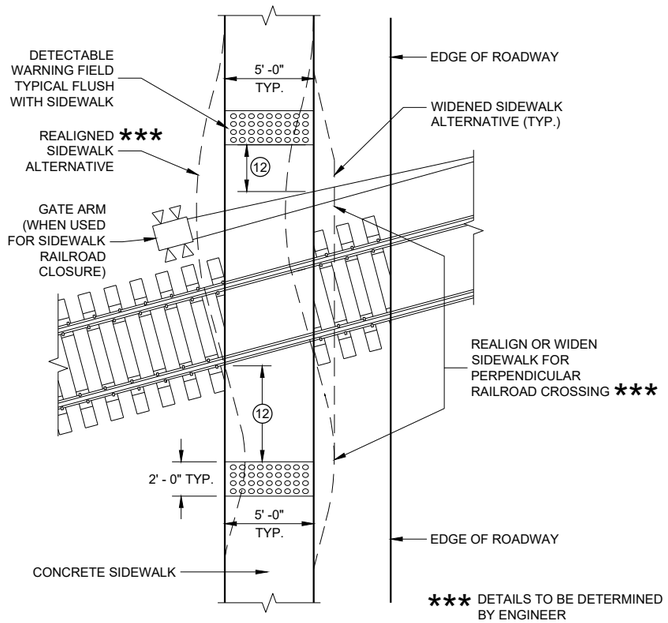
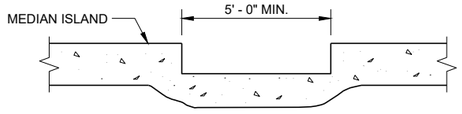
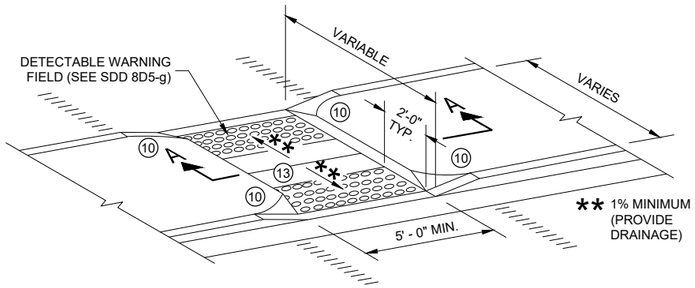


CURB RAMP TYPE 8

DETECTABLE WARNINGS AT RAILROAD CROSSING



SECTION A - A FOR TYPE 5



CURB RAMP TYPE 5

MEDIAN ISLAND NON-ELEVATED PEDESTRIAN CROSSING

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

(2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN ½ - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.

(3) AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.

(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

(6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.

(10) INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

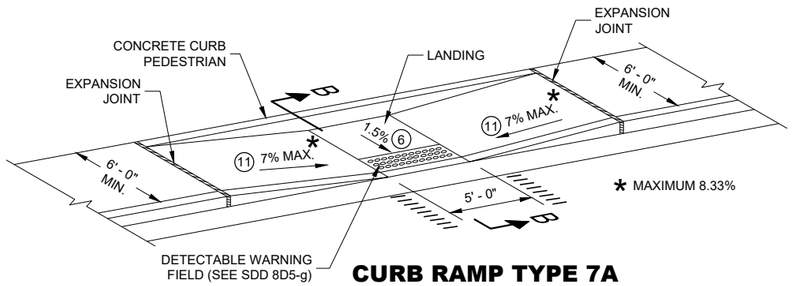
(11) SLOPE SIDEWALK TOWARD LANDING AS SHOWN WHERE THERE IS NO TERRACE OR WHERE THE TERRACE WIDTH IS LESS THAN 6 FEET WIDE.

(12) THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO A RAILROAD CROSSING SHALL BE 1.5 FEET ±0.1' FROM THE FACE OF THE GATE ARM IF THE GATE ARM EXTENDS ACROSS THE SIDEWALK. WHERE THERE IS NO PEDESTRIAN GATE, THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO THE RAILROAD CROSSING SHALL BE 15 FEET FROM THE NEAREST RAIL.

(13) DO NOT INSTALL DETECTABLE WARNING FIELDS AT THE EDGES OF STEET-LEVEL PEDESTRIAN REFUGE ISLANDS IF A MINIMUM 2 FOOT CONCRETE SURFACE WITHOUT DETECTABLE WARNINGS (MEASURED IN THE DIRECTION OF PEDESTRIAN TRAVEL) CANNOT BE ACHIEVED.

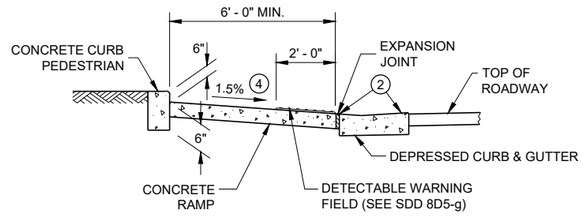
LEGEND

- ½" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)

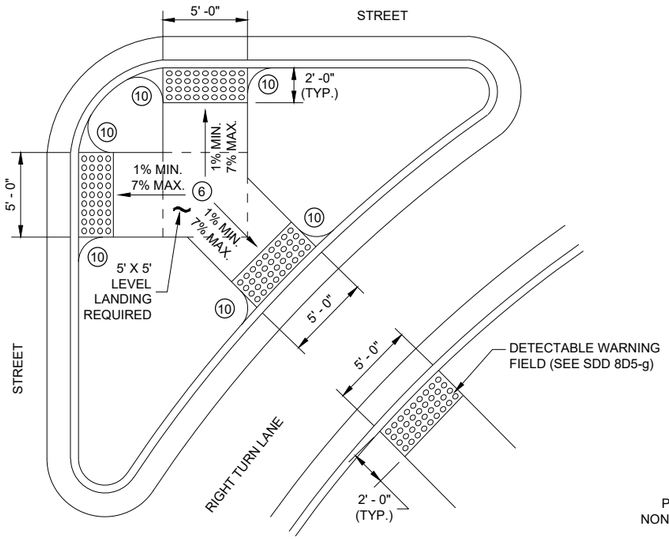


CURB RAMP TYPE 7A

MID BLOCK CROSSING



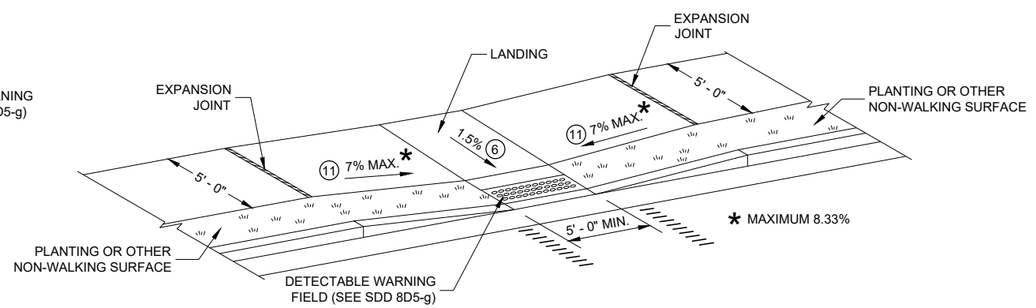
SECTION B - B FOR TYPE 7A



CURB RAMP TYPE 6

DETECTABLE WARNING AT ISLANDS

REFER TO GENERAL NOTES (2) AND (3) FOR ALL ISLAND CURB RAMPS



CURB RAMP TYPE 7B

MID BLOCK CROSSING

NOTE: THESE PARALLEL AND PARALLEL/PERPENDICULAR CURB RAMPS MAY BE USED AT INTERSECTIONS AND MID BLOCK LOCATIONS.

CURB RAMPS TYPE 5, 6, 7A, 7B & 8

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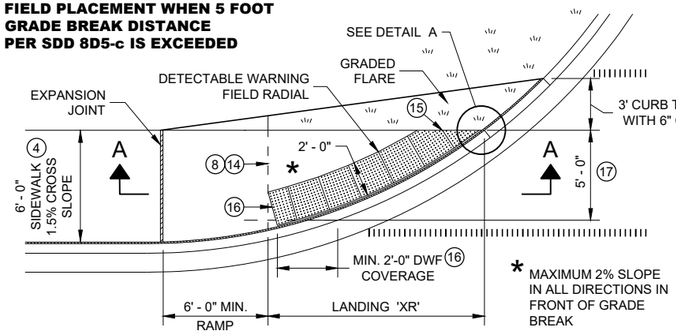
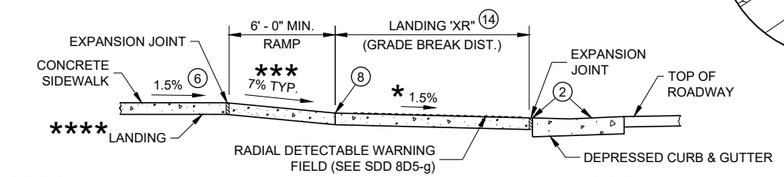
RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-c IS EXCEEDED



PLAN VIEW

CURB RAMP TYPE 4A1

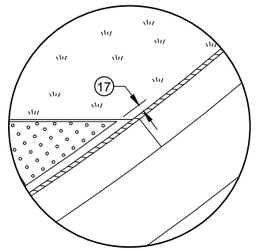
(GRADE BREAK DISTANCE GREATER THAN 5 FEET)



**** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

*** MAXIMUM 8.33%

SECTION A - A FOR TYPE 4A1



DETAIL A

LEGEND

- ½" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT SIDEWALK
- PAVEMENT MARKING CROSSWALK (WHITE)

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

APPLY RADIAL DETECTABLE WARNING PLACEMENT SIMILARLY FOR TYPE 4A AND 4A1 CURB RAMPS AND SIMILARLY FOR TYPE 4B AND 4B1 CURB RAMPS. TYPE 4A AND 4B CURB RAMPS ARE NOT SHOWN.

REFER TO SDD 8D5-g FOR ADDITIONAL RADIAL PLATE REQUIREMENTS.

FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FILED ARE PROHIBITED.

DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AD ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.

(2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN ¼ - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.

(3) AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.

(4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.

(6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET BY 5 FEET.

(8) PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

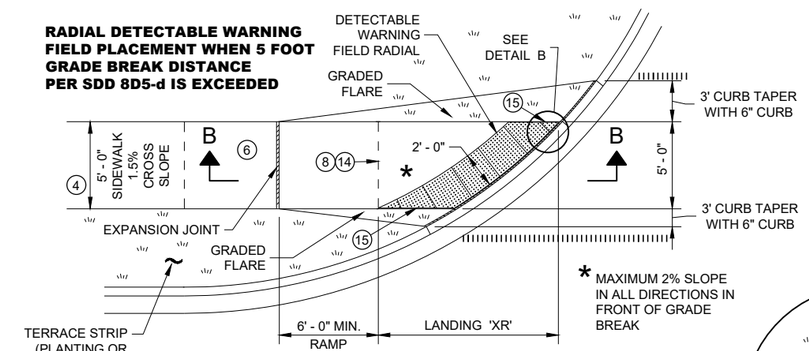
(14) CONSULT ENGINEER IF GRADE BREAK LOCATION (END OF LANDING DIMENSION "XR") REQUIRES FIELD ADJUSTMENT WHEN ESTABLISHING FINAL RADIAL DETECTABLE WARNING FIELD LOCATION.

(15) FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN ⅛" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.

(16) USE 1' X 2" RECTANGULAR END PLATE AT END OF TYPE 4A1 RAMP AND PROVIDE MINIMUM 2' - 0" DETECTABLE WARNING FIELD COVERAGE (IN DIRECTION OF PEDESTRIAN TRAVEL) ALONG THE ENTIRE CURB RAMP WIDTH.

(17) A MAXIMUM 3 INCH CONCRETE BORDER WITH IS ALLOWABLE IN FROM OF RADIAL DETECTABLE WARNING FIELD FOR CONSTRUCTABILITY PURPOSES. CONCRETE BORDER WIDTH MAY VARY UP TO 1 INCH.

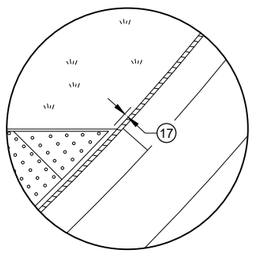
RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-d IS EXCEEDED



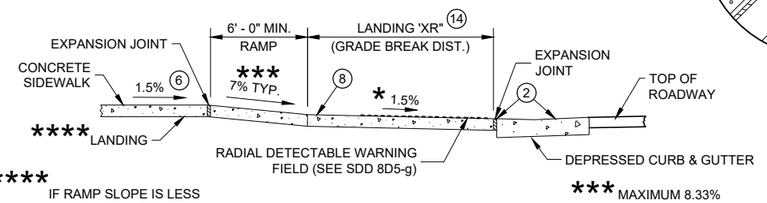
PLAN VIEW

CURB RAMP TYPE 4B1

(GRADE BREAK DISTANCE GREATER THAN 5 FEET)



DETAIL B

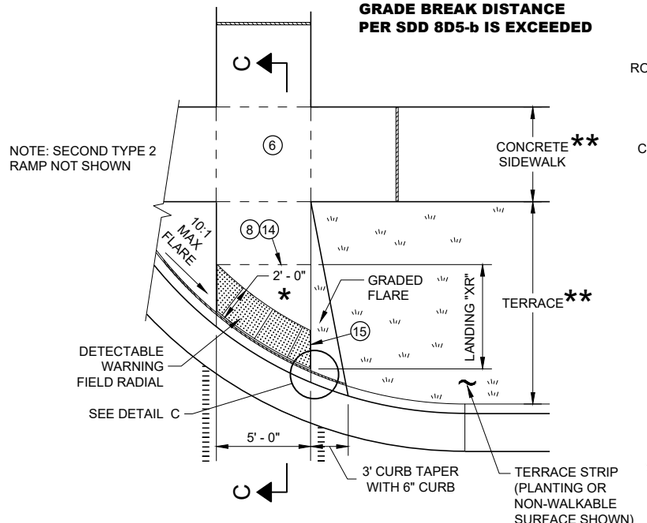


**** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

*** MAXIMUM 8.33%

SECTION B - B FOR TYPE 4B1

RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-b IS EXCEEDED

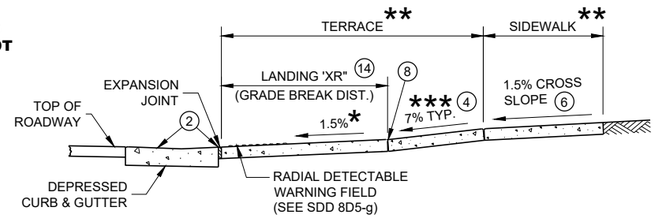


PLAN VIEW

CURB RAMP TYPE 2

(GRADE BREAK DISTANCE GREATER THAN 5 FEET)

(ON LINE WITH SIDEWALK)

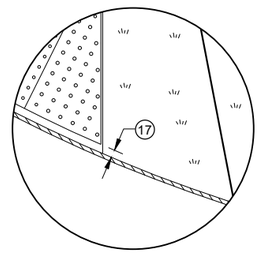


SECTION C - C FOR TYPE 2

* MAXIMUM 2% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK

** WIDTH SHOWN ELSEWHERE IN THE PLANS

*** MAXIMUM 8.33%



DETAIL C

CURB RAMPS RADIAL DETECTABLE WARNING FIELD APPLICATIONS

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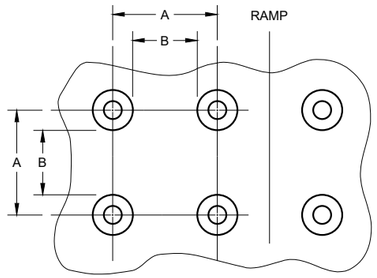
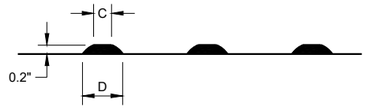
	MIN.	MAX.
A	1.6"	2.4"
B	0.65"	1.5"
C	*	*
D	0.9"	1.4"

* THE C DIMENSION IS 50% TO 65% OF THE D DIMENSION.

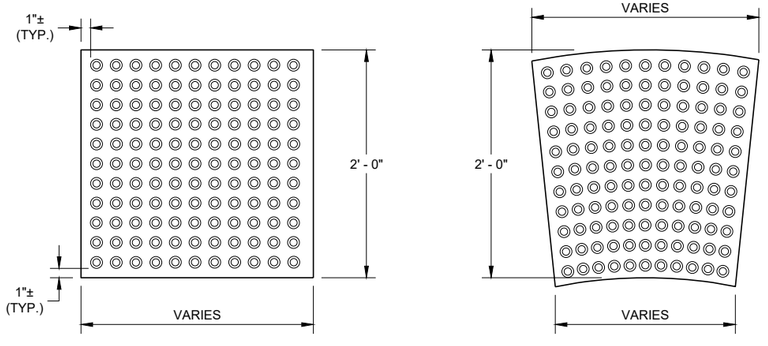


PLAN VIEW



ELEVATION VIEW

TRUNCATED DOMES
DETECTABLE WARNING PATTERN DETAIL



RECTANGULAR PLATES

RADIAL PLATES

PLAN VIEW
DETECTABLE WARNING FIELDS (TYPICAL)

GENERAL NOTES

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AT A CURB RAMP SHALL BE FROM THE SAME MANUFACTURER.

PLACE ALL DETECTABLE WARNING FIELD SYSTEMS IN ACCORDANCE TO THE MANUFACTURER'S RECOMMENDATION.

FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FILED ARE PROHIBITED.

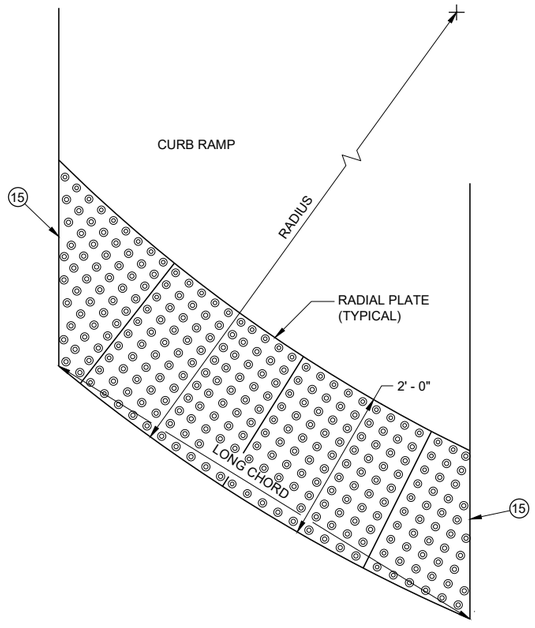
DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.

FOR RADIAL DETECTABLE WARNING FIELD APPLICATIONS WHERE STANDARD RADIAL PLATES ARE NOT AVAILABLE AT AN INTERSECTION CURB RADIUS, A COMBINATION OF SQUARE OR RECTANGULAR PLATES AND RADIAL PLATES MAY BE USED TO FORM RADIAL CONFIGURATION. RADIAL WEDGE PLATES IN COMBINATION WITH SQUARE PLATES ARE ALSO ACCEPTABLE. FOLLOW MANUFACTURER'S RECOMMENDATIONS.

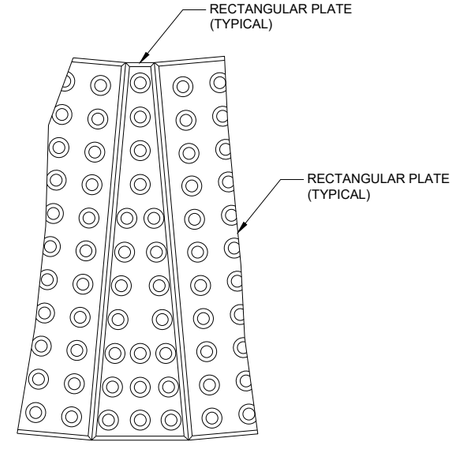
REFER TO CONTRACT AND STANDARD SPECIFICATIONS FOR FIELD CUTTING REQUIREMENTS.

DO NOT EMBED IN CONCRETE ANY FIELD-CUT PLATES WITH CUT EDGES SHORTER THAN 6 INCHES. CONSULT WITH MANUFACTURER FOR RE-DRILLING AND ANCHORING REQUIREMENTS OF FIELD-CUT PLATES.

(15) FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN ⅛" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.



PLAN VIEW
RADIAL DETECTABLE WARNING FIELD ATTRIBUTES



PLAN VIEW
RADIAL WEDGE PLATE CONNECTION DETAIL

CURB RAMPS
RECTANGULAR AND RADIAL
DETECTABLE WARNING PLATES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2019
DATE
/S/ Rodney Taylor
ROADWAY STANDARDS DEVE UNIT SUPERVISOR

FHWA

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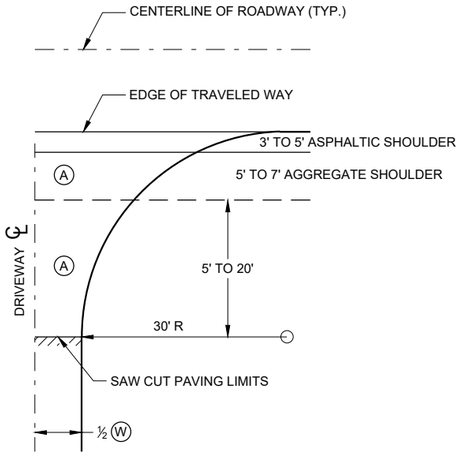
SDD 08D05 - 20g

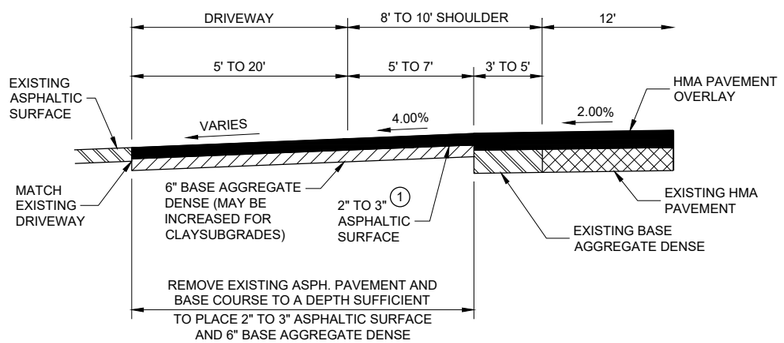
GENERAL NOTES

(1) DESIGN WILL DETERMINE FINAL DRIVEWAY ASPHALTIC THICKNESS BASED ON TYPE OF USAGE AND LOADINGS.



PLAN VIEW

HALF SECTION



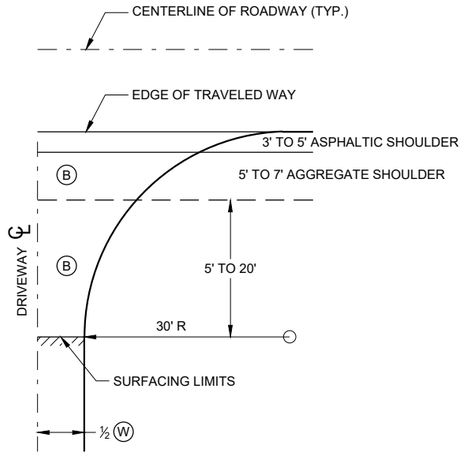
PROFILE VIEW

RURAL ENTRANCE
WITH ASPHALTIC SURFACE
RESURFACING PROJECTS

(A) : PAID FOR AS ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES. (TON)

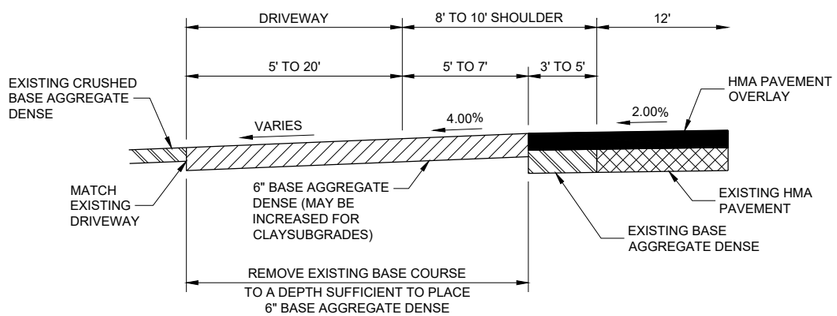
(B) : PAID FOR AS BASE AGGREGATE DENSE 1 ¼" (TON)

(W) : DRIVEWAY WIDTH 16' MIN. - 24' MAX.



PLAN VIEW

HALF SECTION



PROFILE VIEW

RURAL ENTRANCE
WITH AGGREGATE SURFACE
6" BASE AGGREGATE DENSE
RESURFACING PROJECTS

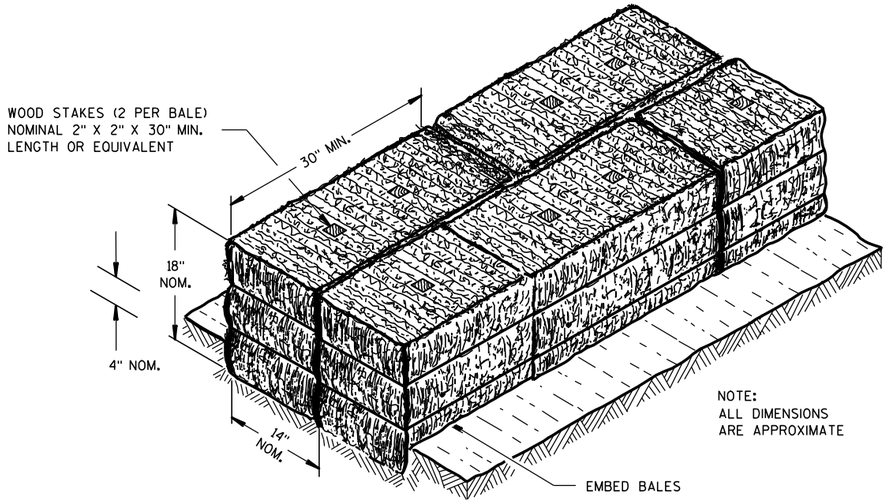
DRIVEWAYS WITHOUT CURB AND GUTTER RESURFACING PROJECTS RURAL

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

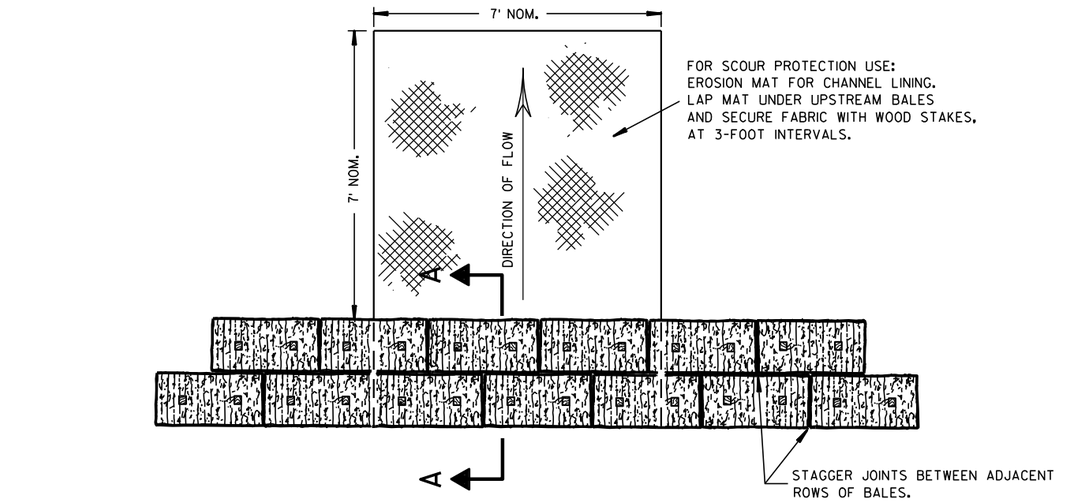
APPROVED
December 2016 — DATE
/S/ Rodney Taylor — ROADWAY STANDARDS DEVE ENGINEER
FHWA

6

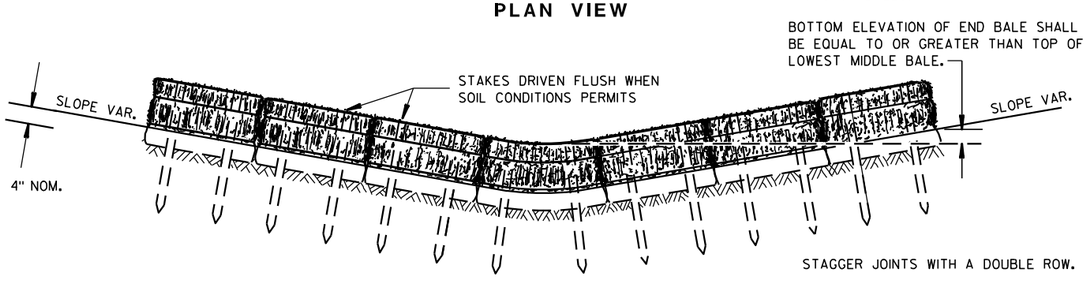
SDD 08D22 - 01
SDD 08D22 - 01



SECTION A-A



PLAN VIEW



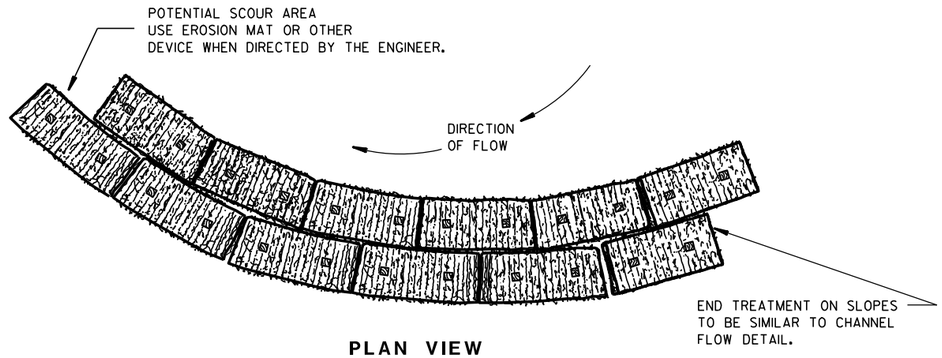
FRONT ELEVATION

TEMPORARY DITCH CHECK USING EROSION BALES (1)

GENERAL NOTES

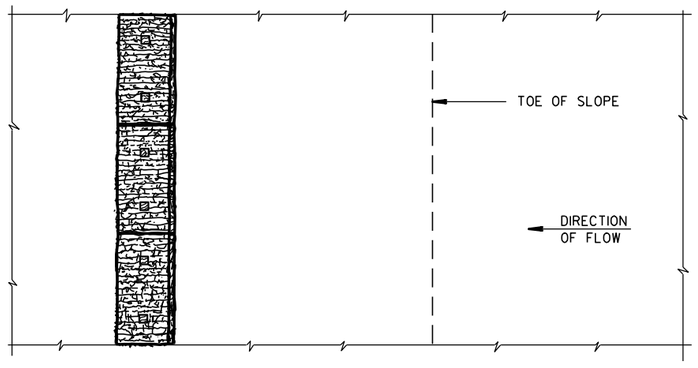
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

(1) TEMPORARY DITCH CHECKS EITHER EROSION BALES OR MANUFACTURED SHALL BE PAID FOR UNDER THE BID ITEM OF TEMPORARY DITCH CHECK. THE DEPARTMENT WILL NOT PAY FOR TEMPORARY DITCH CHECKS CONSTRUCTED OF A SINGLE ROW OF EROSION BALES.

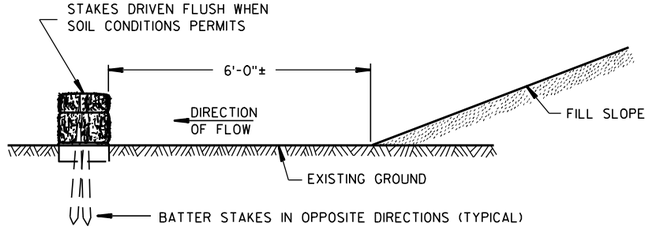


PLAN VIEW

WHEN ALTERING THE DIRECTION OF FLOW



PLAN VIEW



FRONT ELEVATION

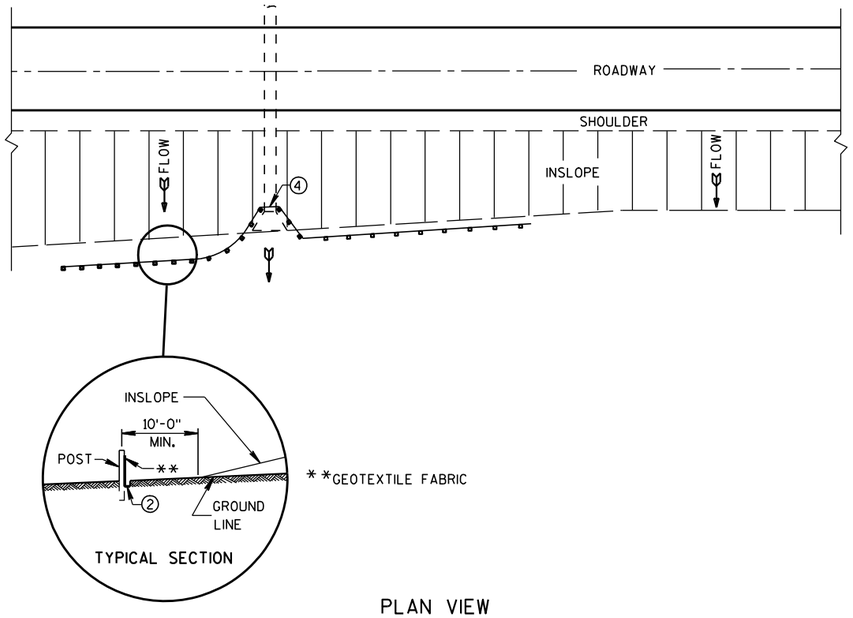
WHEN EXISTING GROUND SLOPES AWAY FROM FILL SLOPE

EROSION BALES FOR SHEET FLOW

TYPICAL INSTALLATIONS OF EROSION BALES / TEMPORARY DITCH CHECKS

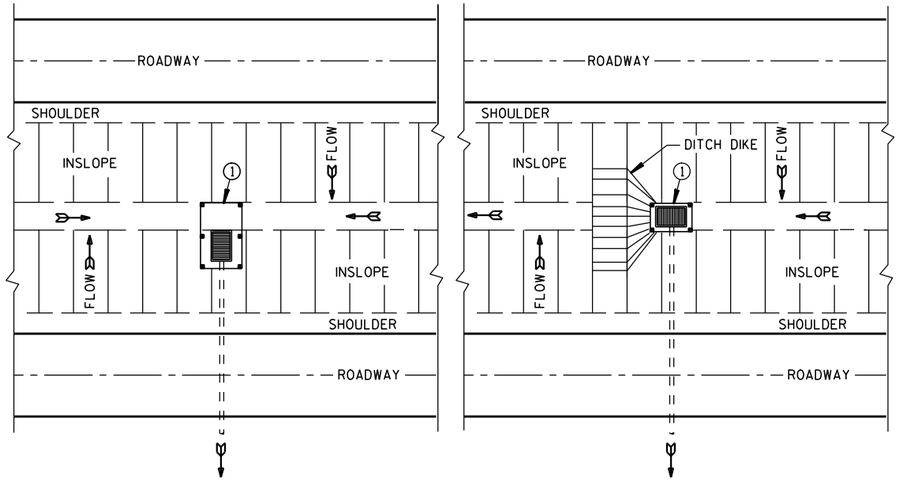
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
6/04/02 DATE
/S/ Beth Cannestra
CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA



PLAN VIEW

TYPICAL APPLICATION OF SILT FENCE



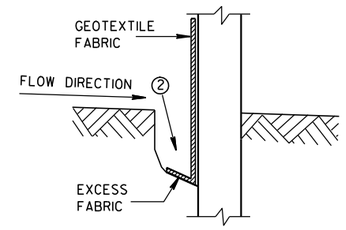
PLAN VIEW

SILT FENCE AT MEDIAN SURFACE DRAINS

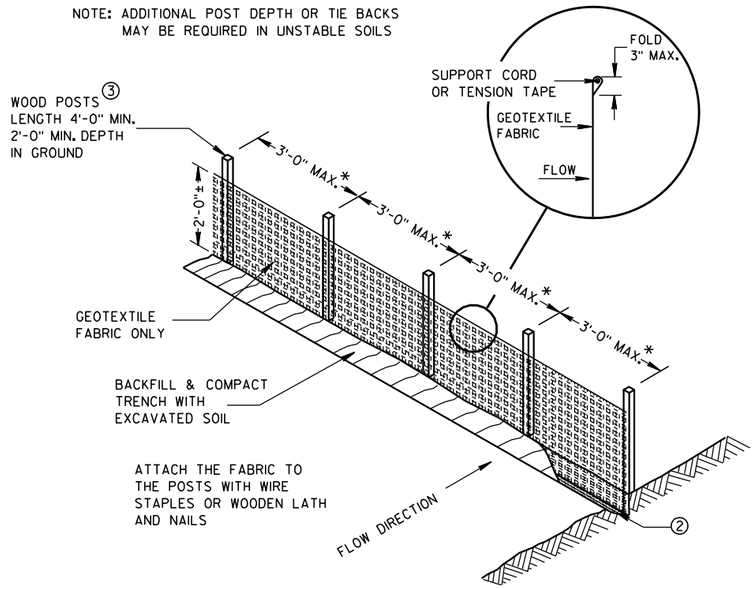
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

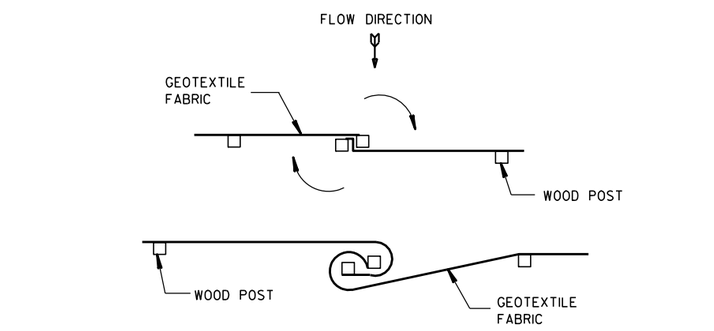
1. HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
2. FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
3. WOOD POSTS SHALL BE A MINIMUM SIZE OF 1⅛" X 1⅛" OF OAK OR HICKORY.
4. SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
5. CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



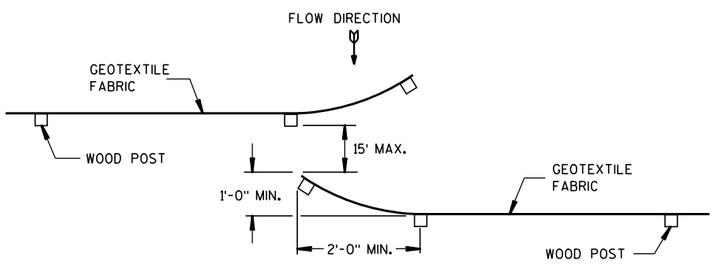
TRENCH DETAIL



SILT FENCE

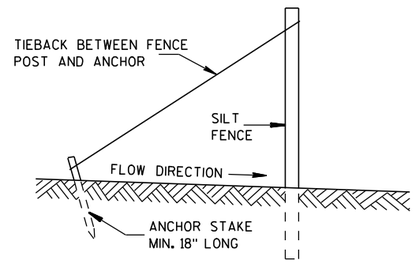


TWIST METHOD



HOOK METHOD

JOINING TWO LENGTHS OF SILT FENCE 5



SILT FENCE TIE BACK

(WHEN REQUIRED BY THE ENGINEER)

SILT FENCE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
4-29-05 DATE
/S/ Beth Cannestra
CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA

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S.D.D. 8 E 9-6

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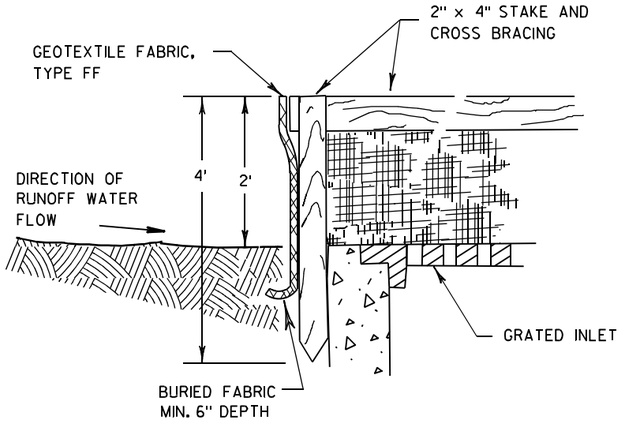
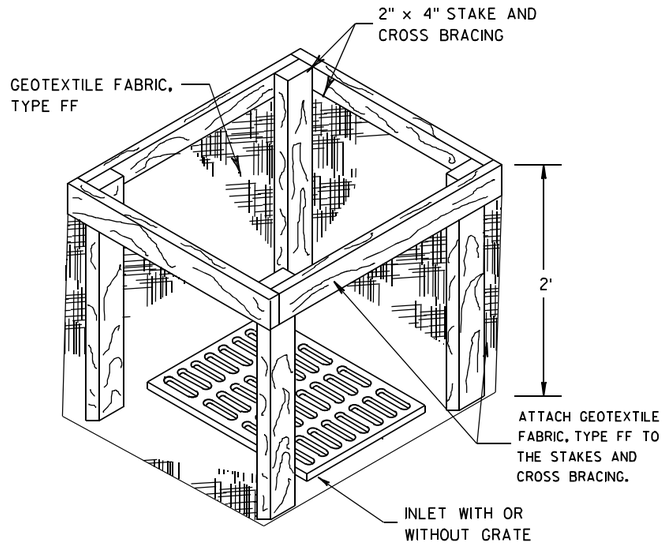
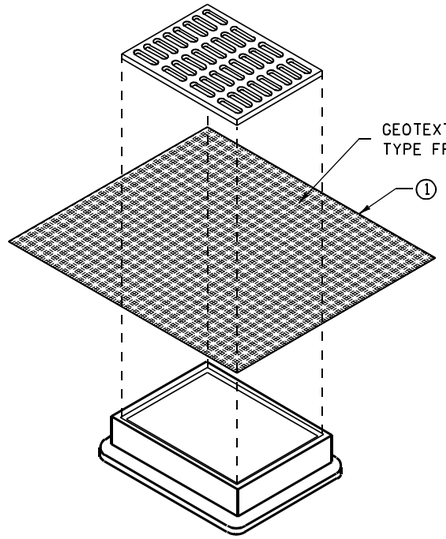
S.D.D. 8 E 9-6



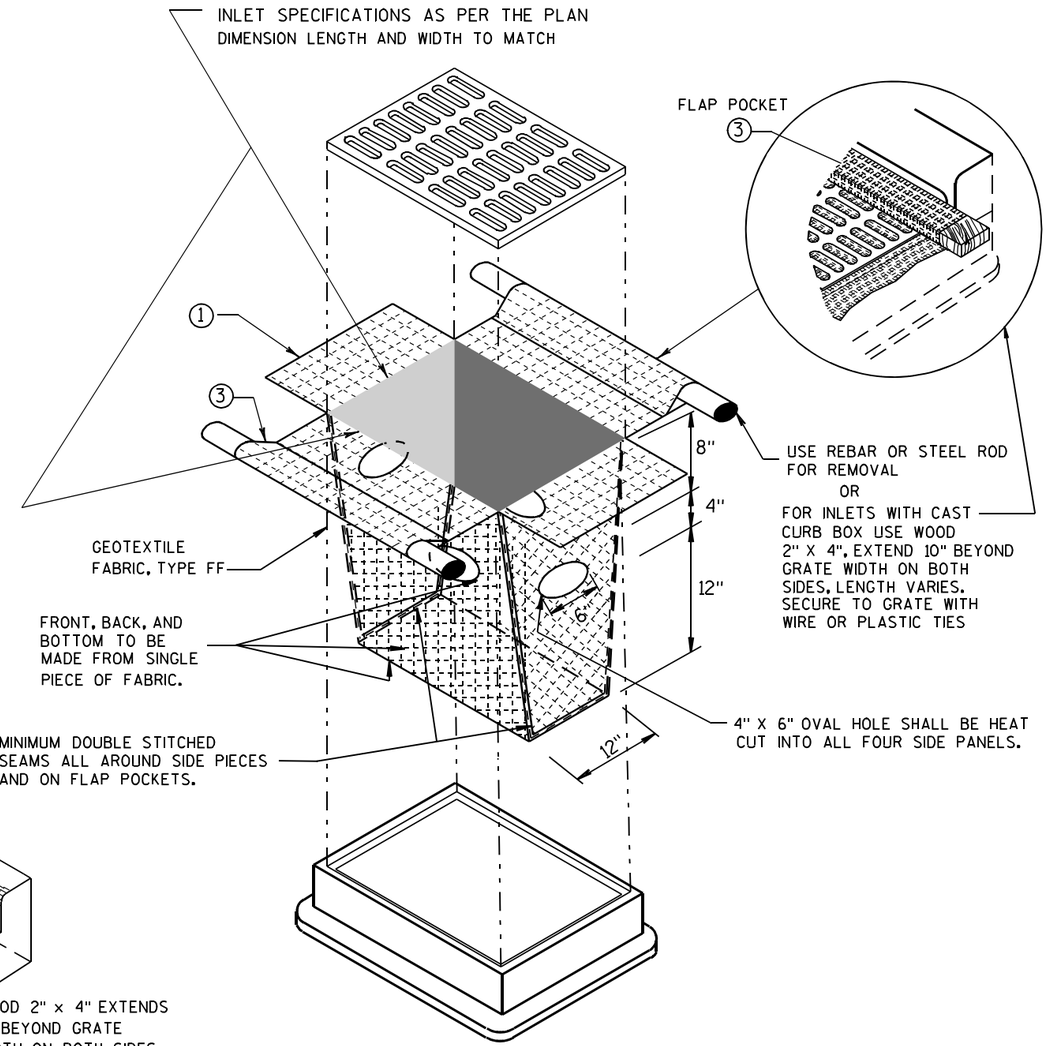


INLET PROTECTION, TYPE A



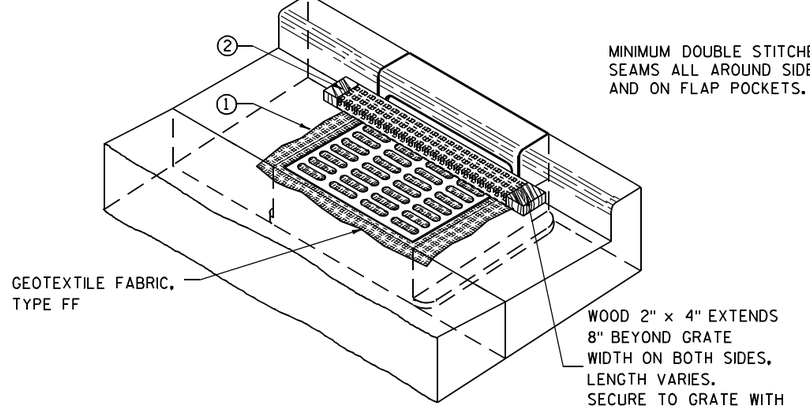
INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE D

(CAN BE INSTALLED IN ANY INLET TYPE WITH
OR WITHOUT A CURB BOX AS PER NOTE ②)



INLET PROTECTION, TYPE C (WITH CURB BOX)

GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.

② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.

③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.

INLET PROTECTION
TYPE A, B, C, AND D

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

10/16/02
DATE

/S/ Beth Cannestra
CHIEF ROADWAY DEVELOPMENT ENGINEER

FHWA

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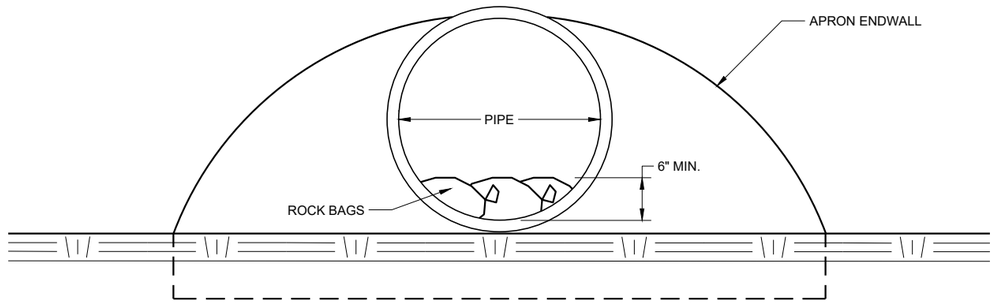
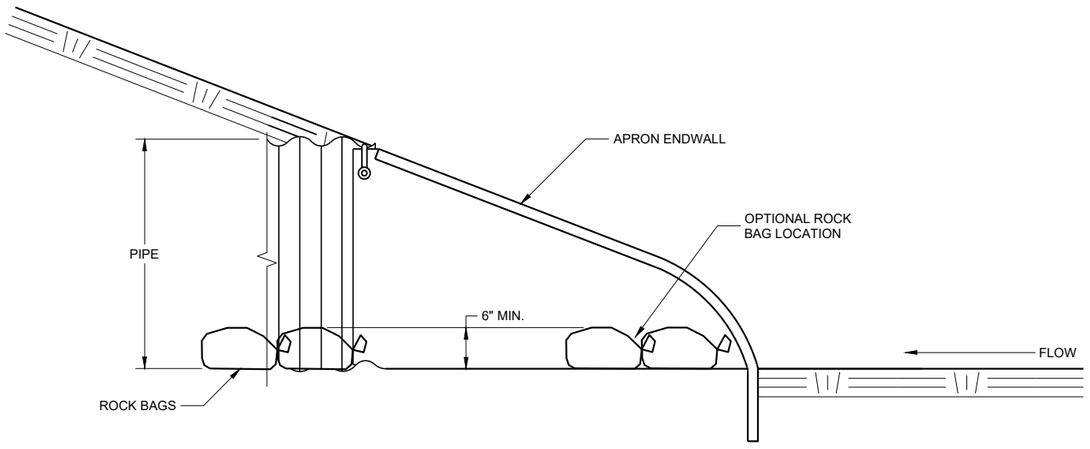
S.D.D. 8 E 10-2



END VIEW



SIDE VIEW

CULVERT PIPE CHECK

(INSTALL ON INLET END ONLY)

CULVERT PIPE CHECK

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2019
DATE

/S/ Daniel Schave
EROSION CONTROL ENGI

FHWA

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SDD 08E15 - 01

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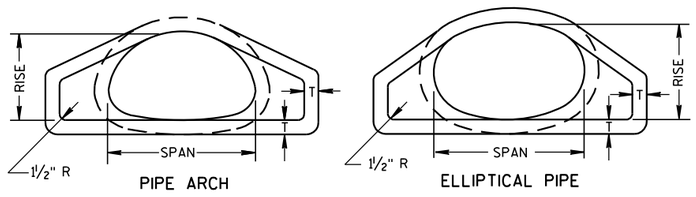
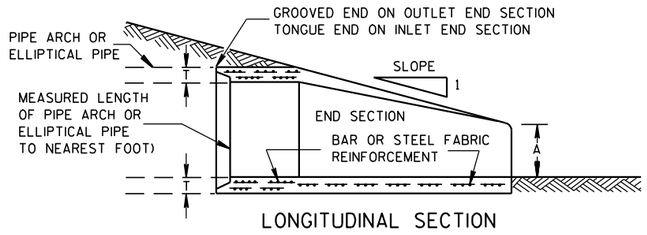
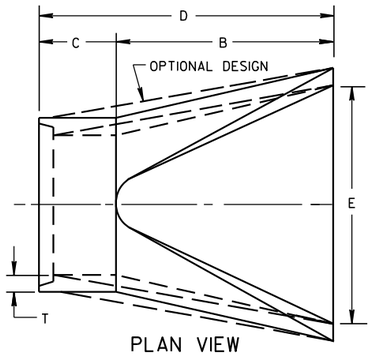
SDD 08E15 - 01



END VIEW

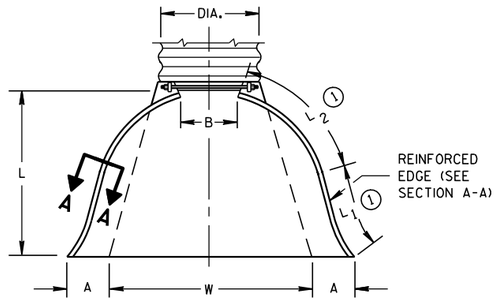


LONGITUDINAL SECTION

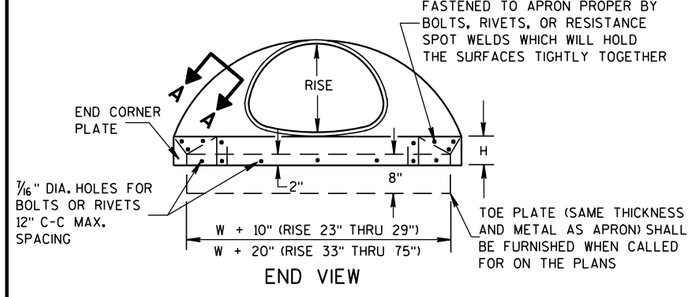


PLAN VIEW

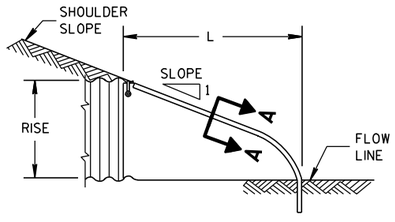
CONCRETE ENDWALLS



PLAN VIEW

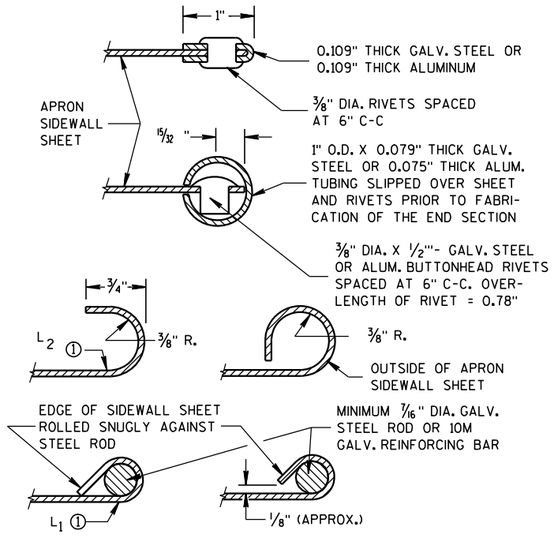


END VIEW



SIDE ELEVATION

METAL ENDWALLS



SECTION A-A

2- 2/3" X 1/2" CORRUGATIONS

EQUIV. DIA. (Inches)	(Inches) SPAN	(Inches) RISE	MIN. THICK. (Inches) STEEL	MIN. THICK. (Inches) ALUM.	DIMENSIONS (Inches) A (±1")	B (MAX.)	H (±1")	L (±1 1/2")	L1 ①	L2 ①	W (±2")	APPROX. SLOPE	BODY
15	17	13	.064	.060	7	9	6	19	14	16	30	2½ to 1	1 Pc.
18	21	15	.064	.060	7	10	6	23	14	19⅜	36	2½ to 1	1 Pc.
21	24	18	.064	.060	8	12	6	28	18	21¾	42	2½ to 1	1 Pc.
24	28	20	.064	.060	9	14	6	32	18	27½	48	2½ to 1	1 Pc.
30	35	24	.079	.075	10	16	6	39	18	37⅝	60	2½ to 1	1 Pc.
36	42	29	.079	.075	12	18	8	46	24	45⅜	75	2½ to 1	1 Pc.
42	49	33	.109	.105	13	21	9	53	24	54¾	85	2½ to 1	2 Pc.
48	57	38	.109	.105	18	26	12	63	24	68	90	2½ to 1	3 Pc.
54	64	43	.109	.105	18	30	12	70	24	72¾	102	2¼ to 1	3 Pc.
60	71	47	.109*	.105*	18	33	12	77	30	82¼	114	2¼ to 1	3 Pc.
66	77	52	.109*	.105*	18	36	12	77	—	—	126	2 to 1	3 Pc.
72	83	57	.109*	.105*	18	39	12	77	—	—	138	2 to 1	3 Pc.

3" X 1" CORRUGATIONS

EQUIV. DIA. (Inches)	(Inches) SPAN	(Inches) RISE	MIN. THICK. (Inches) STEEL	MIN. THICK. (Inches) ALUM.	DIMENSIONS (Inches) A (±1")	B (MAX.)	H (±1")	L (±1 1/2")	L1 ①	L2 ①	W (±2")	APPROX. SLOPE	BODY
48	53	41	.109	.105	18	26	12	63	24	72¾	90	2½ to 1	2 Pc.
54	60	46	.109	.105	18	30	12	70	30	82¼	102	2 to 1	2 Pc.
60	66	51	.109*	.105*	18	33	12	77	—	—	114	1½ to 1	3 Pc.
66	73	55	.109*	.105*	18	36	12	77	—	—	126	1½ to 1	3 Pc.
72	81	59	.109*	.105*	18	39	12	77	—	—	138	2 to 1	3 Pc.
78	87	63	.109*	.105*	22	38	12	77	—	—	148	1½ to 1	3 Pc.
84	95	67	.109*	.105*	22	34	12	77	—	—	162	1½ to 1	3 Pc.
90	103	71	.109*	.105*	22	38	12	77	—	—	174	1½ to 1	3 Pc.
96	112	75	.109*	.105*	24	40	12	77	—	—	174	1½ to 1	3 Pc.

NOTE: ALL SPLICES TO BE LAP RIVETED OR BOLTED.

* EXCEPT CENTER PANEL SEE GENERAL NOTES

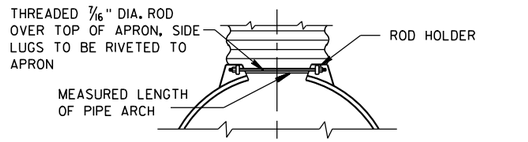
REINFORCED CONCRETE PIPE ARCH

EQUIV. DIA. (Inches)	DIMENSIONS (Inches) ** SPAN	** RISE	T	A	B	C	D	E	APPROX. SLOPE
24	29	18	3	8½	39	33	72	48	3 to 1
30	36	22	3½	9½	50	46	96	60	3 to 1
36	44	27	4	11⅛	60	36	96	72	3 to 1
42	51	31	4½	15 13/16	60	36	96	78	3 to 1
48	58	36	5	21	60	36	96	84	3 to 1
54	65	40	5½	25½	60	36	96	90	3 to 1
60	73	45	6	31	60	36	96	96	3 to 1
72	88	54	7	31	60	39	99	120	2 to 1
84	102	62	8	28½	83	19	102	144	2 to 1

REINFORCED CONCRETE ELLIPTICAL PIPE

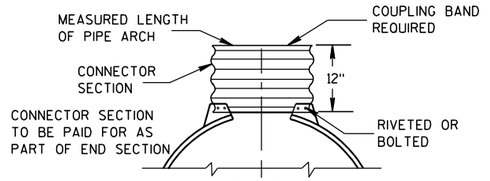
EQUIV. DIA. (Inches)	DIMENSIONS (Inches) ** SPAN	** RISE	T	A	B	C	D	E	APPROX. SLOPE
24	30	19	3¼	8½	39	33	72	48	3 to 1
30	38	24	3¾	9½	54	18	72	60	3 to 1
36	45	29	4½	11⅛	60	24	84	72	2½ to 1
42	53	34	5	15¾	60	36	96	78	2½ to 1
48	60	38	5½	21	60	36	96	84	2½ to 1
54	68	43	6	25½	60	36	96	90	2½ to 1
60	76	48	6½	30	60	36	96	96	2½ to 1

**NOMINAL SIZE



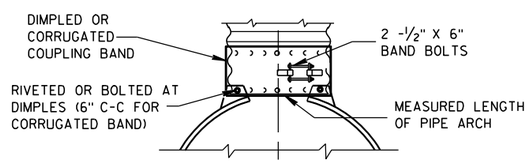
TYPE 2

FOR 17" X 13" THRU 112" X 75" PIPE ARCH



TYPE 3

FOR 64" X 43" THRU 112" X 75" PIPE ARCH



TYPE 5

ALTERNATE FOR:

ALL SIZES CORRUGATED PIPE ARCHES

NOTE: DIMPLED BAND FITS OVER OUTSIDE OF ENDWALL, AND CORRUGATED BAND FITS INSIDE ENDWALL.

CONNECTION DETAILS

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

CONCRETE APRON ENDWALLS MAY NOT BE USED WITH GALVANIZED STEEL OR ALUMINUM CULVERT PIPE OR VISE VERSA. GALVANIZED STEEL OR ALUMINUM APRON ENDWALLS SHALL NORMALLY BE INSTALLED ON CULVERT PIPE OF THE SAME METAL.

ALL THREE PIECE STEEL APRON ENDWALLS FOR 66" X 51" PIPE ARCH AND LARGER SHALL HAVE 0.109" SIDES AND 0.138" CENTER PANELS. ALL THREE PIECE ALUMINUM APRON ENDWALLS FOR 66" X 51" PIPE ARCH AND LARGER SHALL HAVE 0.105" SIDES AND 0.134" CENTER PANELS. THE WIDTH OF CENTER PANELS SHALL BE GREATER THAN 20 PERCENT OF THE PIPE ARCH PERIMETER.

LAP SEAMS SHALL BE TIGHTLY JOINED BY GALVANIZED RIVETS OR BOLTS FOR STEEL UNITS AND ALUMINUM RIVETS AND BOLTS FOR ALUMINUM UNITS. FOR THE 77" X 52" THROUGH 112" X 75" APRON ENDWALL SIZES, THE REINFORCED EDGES AND CENTER PANEL SEAMS SHALL BE FURTHER REINFORCED WITH GALVANIZED STEEL OR ALUMINUM STIFFENER ANGLES. THE ANGLES SHALL BE ATTACHED BY GALVANIZED NUTS AND BOLTS FOR STEEL UNITS AND ALUMINUM NUTS AND BOLTS FOR ALUMINUM UNITS.

WHERE TWO OR MORE PIPES WITH APRON ENDWALLS ARE LAID ADJACENT TO EACH OTHER, THEY SHALL BE SEPARATED BY A DISTANCE SUFFICIENT TO PROVIDE A MINIMUM CLEARANCE OF 6 INCHES BETWEEN APRON ENDWALLS.

① FOR PIPE ARCH SIZES UP TO 73" X 55" A 180° ROLLED EDGE MAY BE USED INSTEAD OF STEEL ROD REINFORCEMENT. SEE SECTION A-A.

APRON ENDWALLS FOR PIPE ARCH AND ELLIPTICAL PIPE

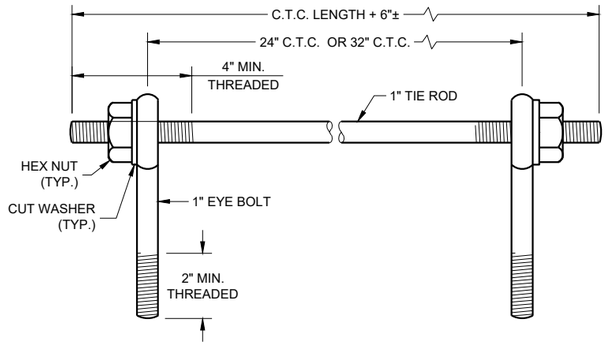
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

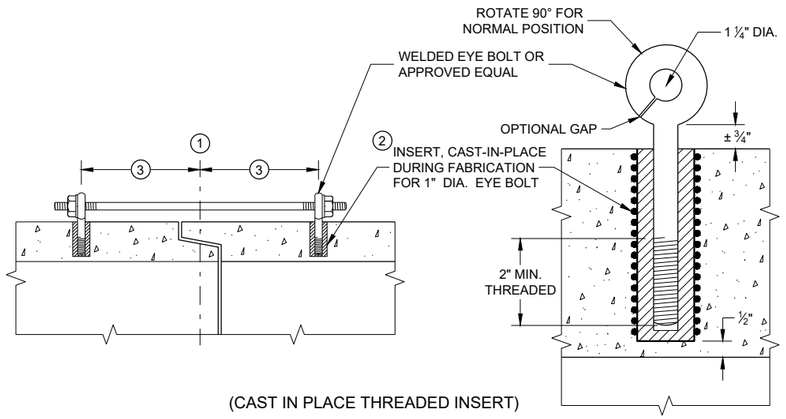
11/30/94 — DATE

/S/ Rory L. Rhinesmith — CHIEF ROADWAY DEVELOPMENT ENGINEER

FHWA



EYE BOLTS AND TIE ROD



(CAST IN PLACE THREADED INSERT)

LONGITUDINAL SECTIONS

EYE BOLT AND TIE ROD ASSEMBLY (ALTERNATE NO. 1)

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS, AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

CONCRETE CULVERT AND STORM SEWER PIPE SHALL BE TIED TOGETHER IN THE MANNER ILLUSTRATED BY THIS DETAIL AT LOCATIONS DESIGNATED IN THE STANDARD SPECIFICATIONS AND THE PLAN. THE CONTRACTOR MAY USE EITHER ALTERNATE 1, 2 OR 3 FOR DRAINAGE STRUCTURES. ONLY ALTERNATE 1 AND 3 MAY BE USED FOR CATTLE PASSES, UNLESS OTHERWISE STATED IN THE CONTRACT. THE MATERIALS, FABRICATION AND WORK NECESSARY TO TIE THE PIPE BY THIS DETAIL WILL BE CONSIDERED INCIDENTAL TO THE PIPE AND APRON ENDWALLS IF REQUIRED.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR JOINT TIES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.

JOINT TIES TO BE HOT-DIP GALVANIZED PER ASTM A 153.

(1) CENTER LINE OF TONGUE AND GROOVE OR BELL AND SPIGOT JOINTS.

(2) THE INSIDE OF THE THREADED INSERTS SHALL BE CLEAN TO ALLOW THE INSERTION OF THREADED EYE BOLTS.

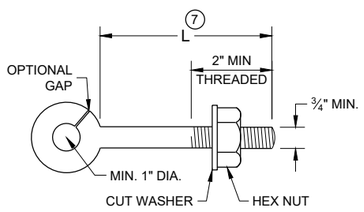
(3) HOLES SHALL BE CAST-IN-PLACE OR DRILLED PER THE APPLICABLE DETAIL, AND EQUAL DISTANCE FROM THE CENTERLINE OF THE JOINT.

(4) BOLT PROJECTION INSIDE OF PIPE SHALL NOT EXCEED 2 INCHES.

(5) OPENING TO BE ROD DIAMETER PLUS 1 INCH.

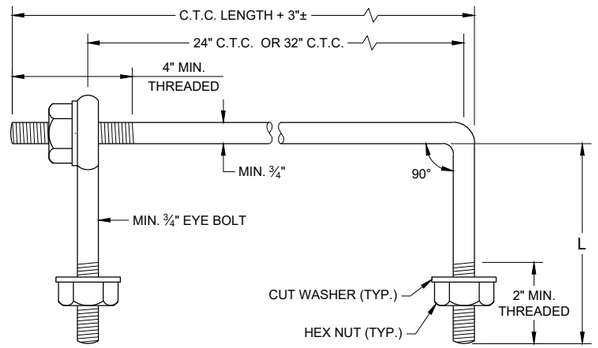
(6) LENGTH ADEQUATE TO EXTEND TO WITHIN ½ INCH OF THE INNER SURFACE OF THE PIPE.

(7) EYE BOLT LENGTH DETERMINED BY WALL THICKNESS, BELL THICKNESS AND BOLT PROJECTION INSIDE PIPE.

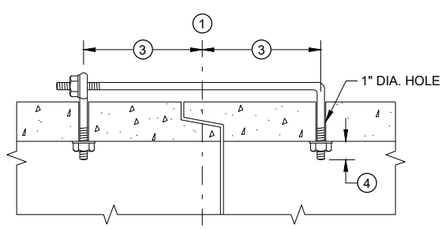


EYE BOLT (7)

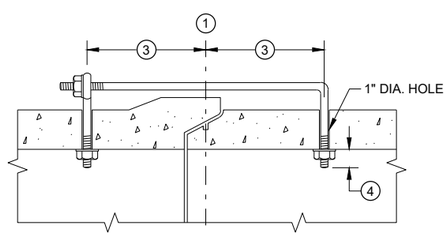
NOTE: TWO EYE BOLTS MAY BE USED WITH A 30" OR 38" LONG THREADED ROD IN LIEU OF THE 90° BENT TIE ROD.



EYE BOLT AND TIE ROD



(TONGUE AND GROOVE PIPE)



(MODIFIED BELL PIPE)

LONGITUDINAL SECTION

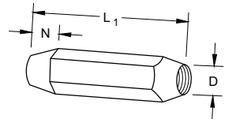
(JOINT TIES FOR 18" TO 66" DIA. CONCRETE PIPE)

EYE BOLT AND TIE ROD ASSEMBLY (ALTERNATE NO. 2)

ADJUSTABLE TIE ROD TABLE

PIPE DIAMETER	TIE ROD DIAMETER	D	L_1	N
12 - 60	⅝	⅝	5	½
66 - 84	¾	¾	5	½
90 - 144	1	1	7	1 7/16

DIMENSIONS SHOWN ARE IN INCHES



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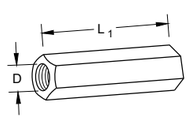
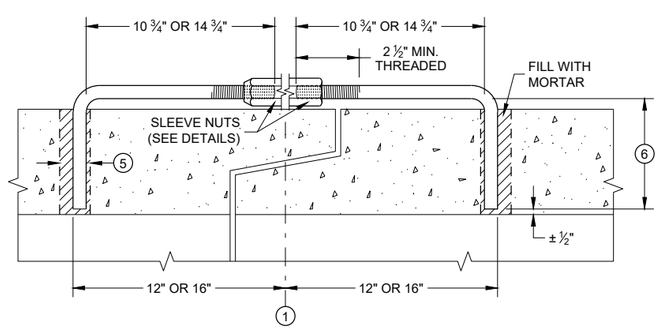


PLAIN

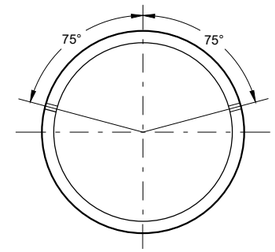
RIGHT AND LEFT THREADS

SLEEVE NUTS



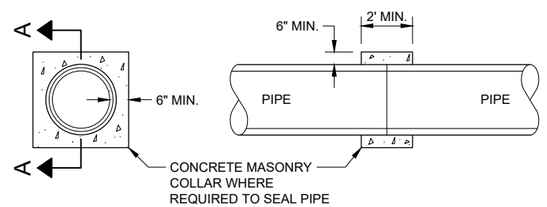
LONGITUDINAL SECTION

ADJUSTABLE TIE ROD (ALTERNATE NO. 3)



PLACEMENT OF (2) CAST-IN-PLACE INSERTS OR HOLES DURING FABRICATION FOR PIPE SECTIONS REQUIRING TIE RODS

TRANSVERSE SECTION



SECTION A - A

CONCRETE COLLAR DETAIL

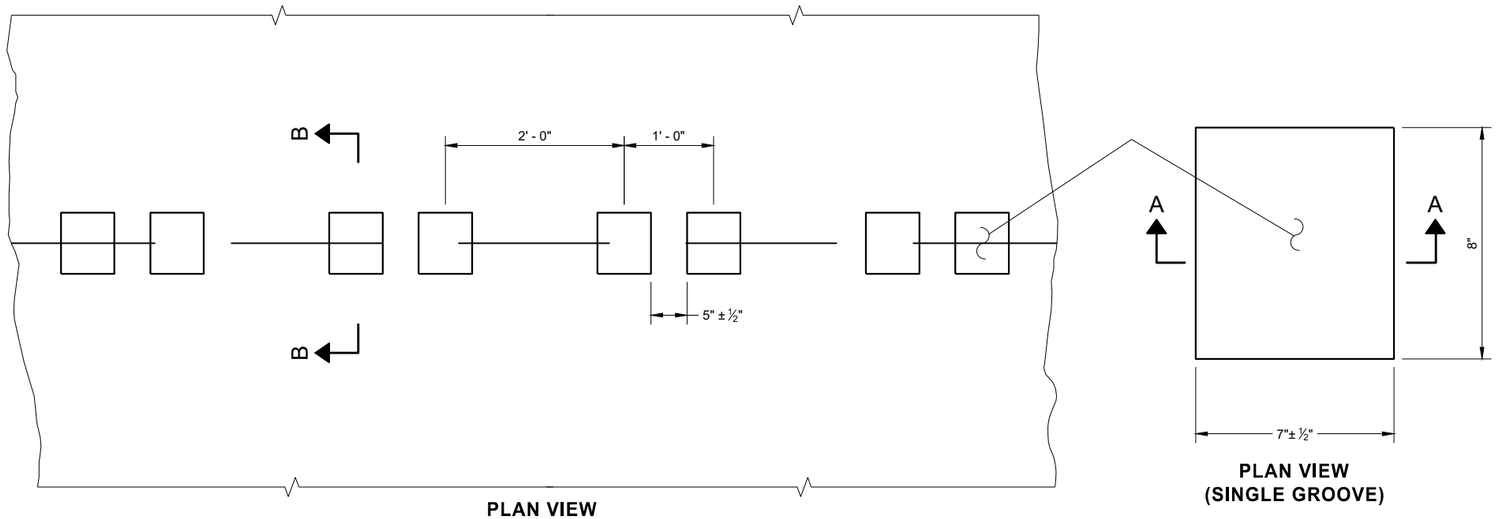
JOINT TIES FOR CONCRETE PIPE AND CONCRETE COLLAR DETAIL

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2021 — DATE
/S/ Rodney Taylor — ROADWAY STANDARDS DEVE ENGINEER
FHWA

SDD 08F04 - 08

SDD 08F04 - 08



PLAN VIEW
SHOULDER WITH GROOVES

PLAN VIEW
(SINGLE GROOVE)

PLACEMENT DETAIL FOR TYPE 1 MILLED RUMBLE STRIP

GENERAL NOTES

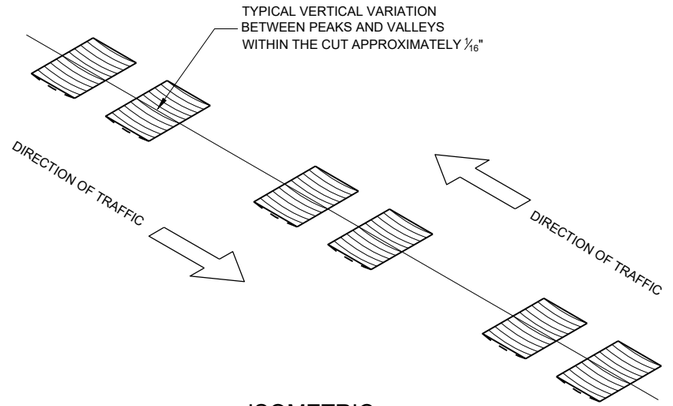
DETAILS OF CONSTRUCTION NO SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

DO NOT MILL CENTERLINE GROOVES THROUGH ANY INTERSECTION, MARKED CROSSWALK, NON-MOTORIZED PATH CROSSING, OR SNOWMOBILE CROSSING.

INSTALL PAVEMENT MARKING AFTER THE GROOVES ARE INSTALLED.

SEE SIGNING PLAN FOR SIGN REQUIREMENTS THAT MAY BE NEEDED.

(1) CENTERLINE GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS. WHEN DIRECTED BY THE ENGINEER.



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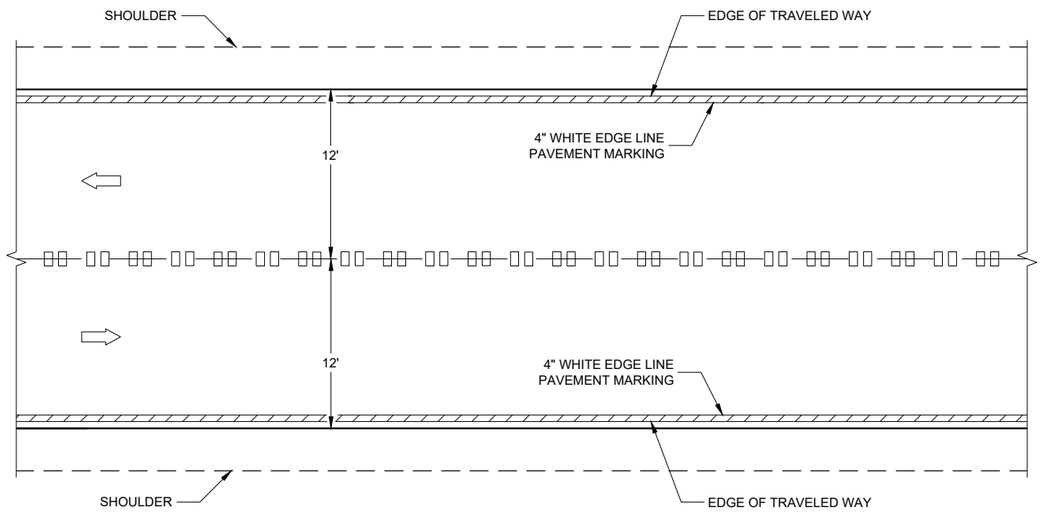
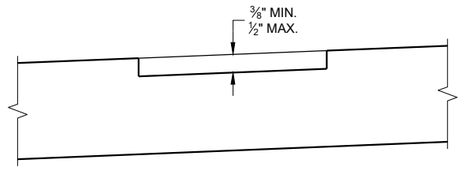
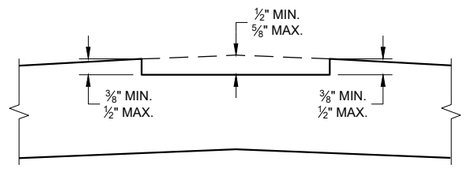


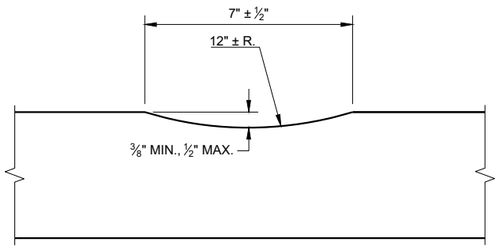
CENTERLINE GROOVES ON TWO-WAY ROADWAYS



SECTION B - B
SUPERELEVATED ROADWAY



SECTION B - B
CROWNED ROADWAY



SECTION A - A

2-LANE RURAL
CENTER LINE RUMBLE STRIP, MILLING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

SDD 13A11 - 03a

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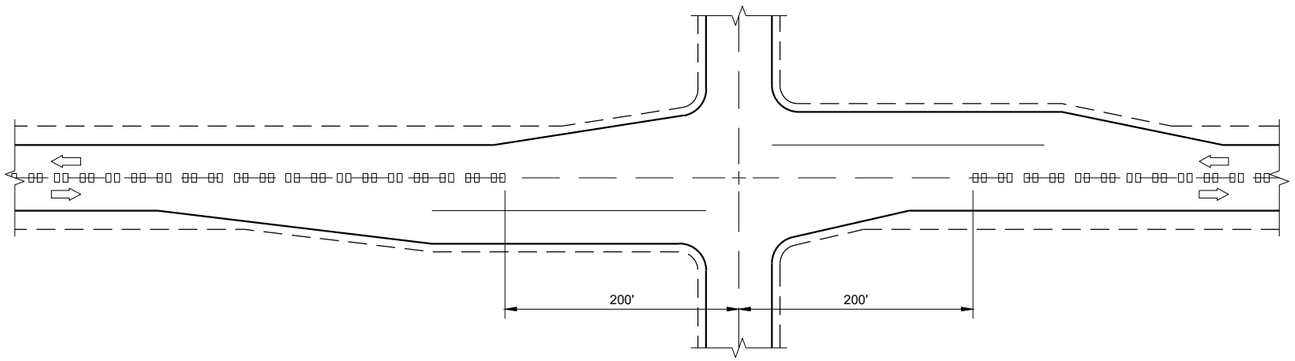
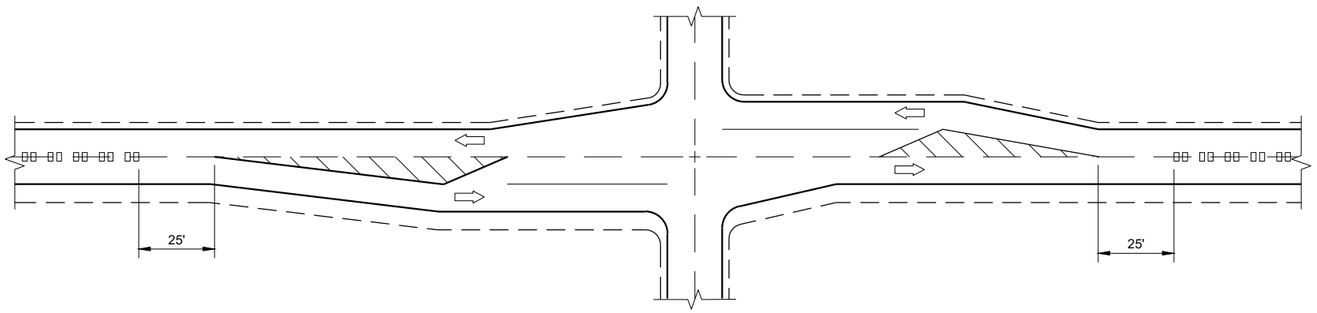
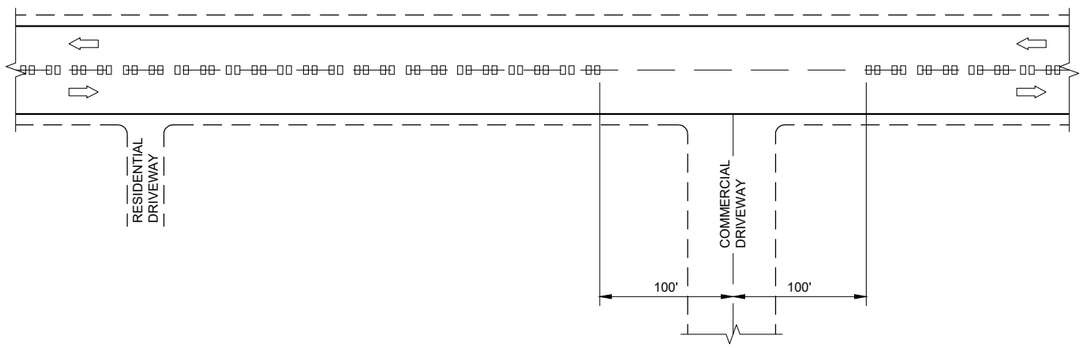
SDD 13A11 - 03a



CENTERLINE GROOVES AT INTERSECTIONS



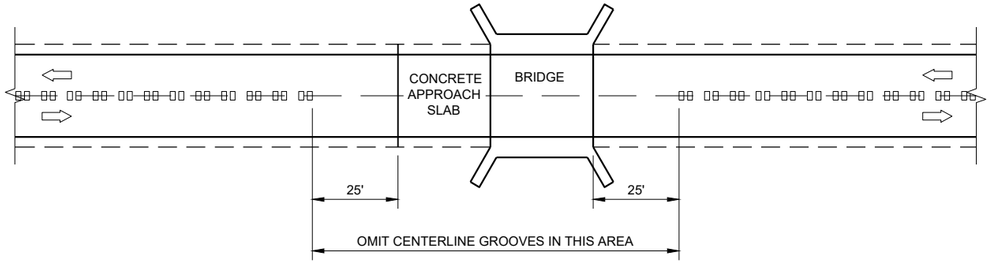
CENTERLINE GROOVES AT INTERSECTIONS (WITH LEFT TURN LANES)



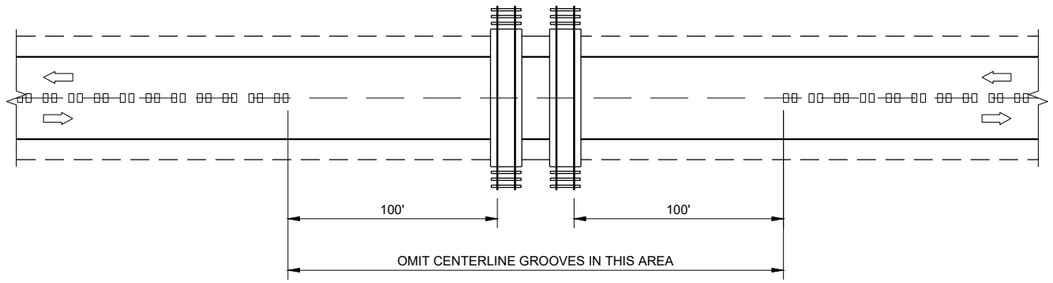
CENTERLINE GROOVES AT DRIVEWAYS (1)

GENERAL NOTES

(1) CENTERLINE GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS. WHEN DIRECTED BY THE ENGINEER.



CENTERLINE GROOVES AT BRIDGES



CENTERLINE GROOVES AT RAILROADS

2-LANE RURAL
CENTERLINE RUMBLE STRIP, MILLING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
7/2018
DATE
/S/ Rodney Taylor
ROADWAY STANDARDS DEVE ENGINEER

FHWA

6

SDD 13A11 - 03b

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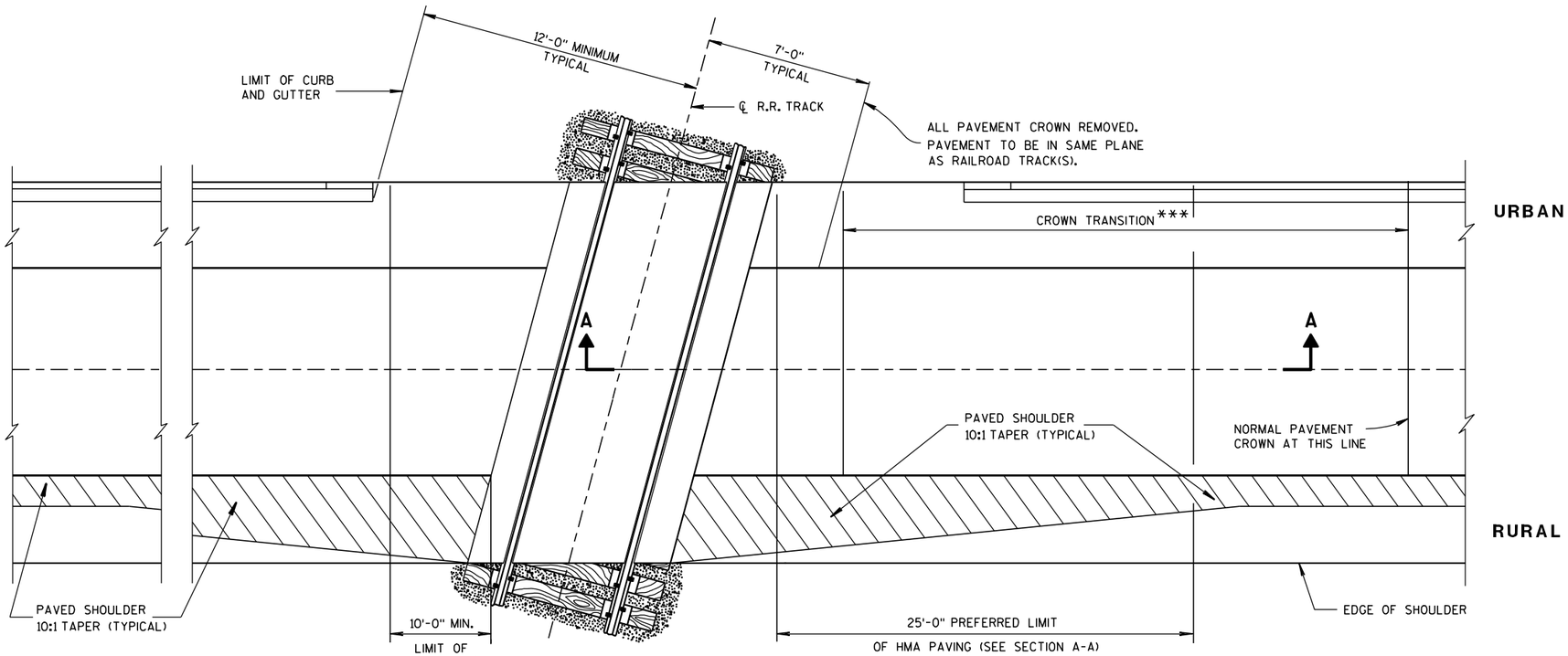
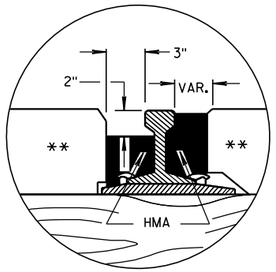
SDD 13A11 - 03b





DETAIL B

HMA FLANGEWAY AND FIELD FILLERS

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

TIMBER, CONCRETE OR RUBBER CROSSING SURFACE MATERIAL, RAILS, TIES, BALLAST, GEOTEXTILE FABRIC AND CROSSING DRAINAGE SYSTEM BY OTHERS UNLESS OTHERWISE PROVIDED.

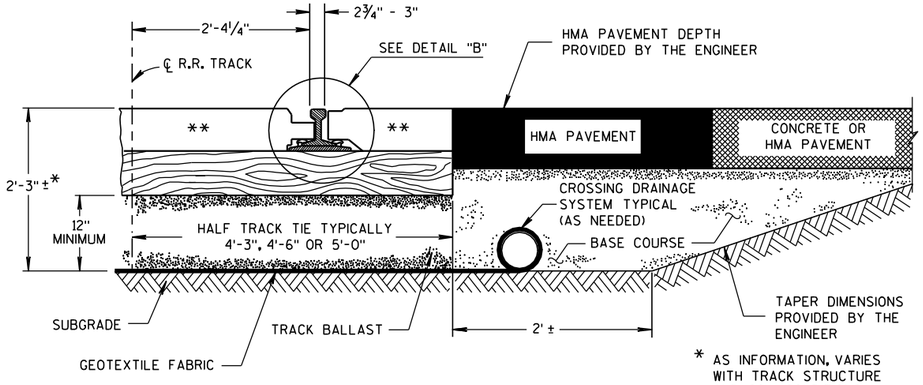
HMA PAVEMENT APPROACHES AND HMA PAVEMET CROSSING SURFACES TO BE PLACED BY CONTRACTOR UNLESS OTHERWISE PROVIDED.

HMA FLANGEWAY AND FIELD FILLERS TO BE PLACED AND THOROUGHLY HAND COMPACTED BY THE CONTRACTOR WHEN NOT PROVIDED BY OTHERS. SEE DETAIL B. HMA FILLERS NOT REQUIRED WHEN RUBBER FILLERS ARE PROVIDED.

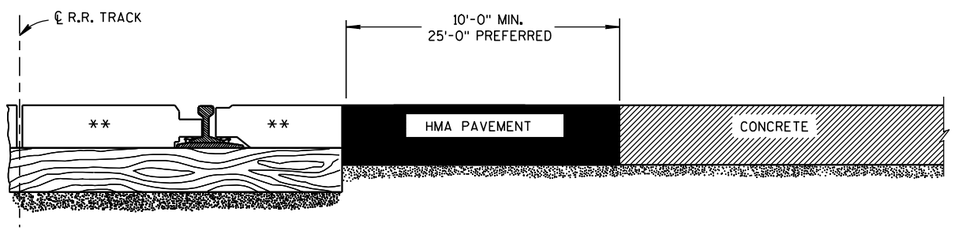
HMA PAVEMENT SHALL BE ROLLED PARALLEL TO THE TRACK.

** CROSSING SURFACE MAY BE TIMBER, RUBBER, CONCRETE, HMA PAVEMENT OR A COMBINATION OF SUCH MATERIALS.

*** CROWN TRANSITION LENGTH SHOWN ELSEWHERE IN THE PLAN.



TYPICAL HALF SECTION



SECTION A-A

CONCRETE PAVEMENT APPROACH

SECTION A-A

HMA PAVEMENT APPROACH

EXAMPLES OF PAVEMENT APPROACHES

PAVEMENT DETAILS FOR RAILROAD APPROACH

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
8-28-09 DATE
/S/ Ronald E. Adams
CHIEF, RAILROADS & HAI CTION
FHWA

6

S.D.D. 13 B 1-10

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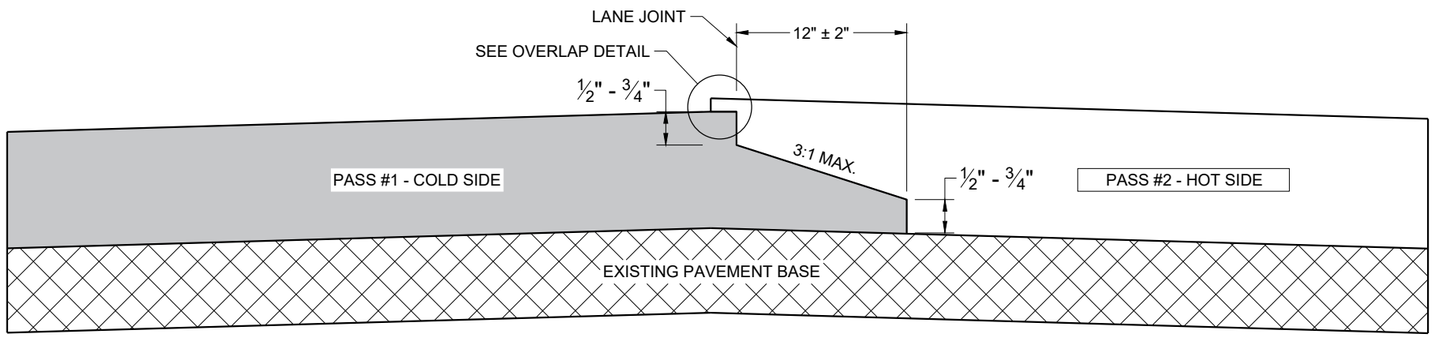
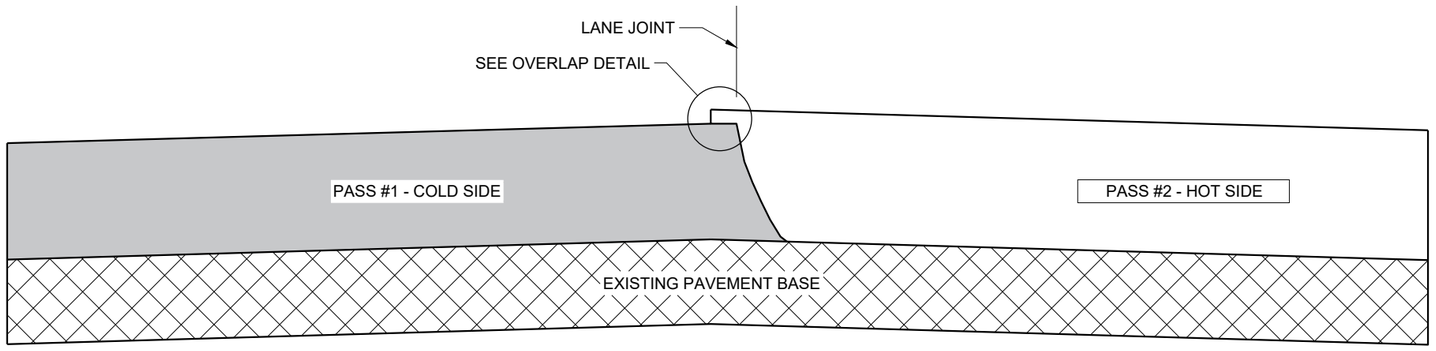
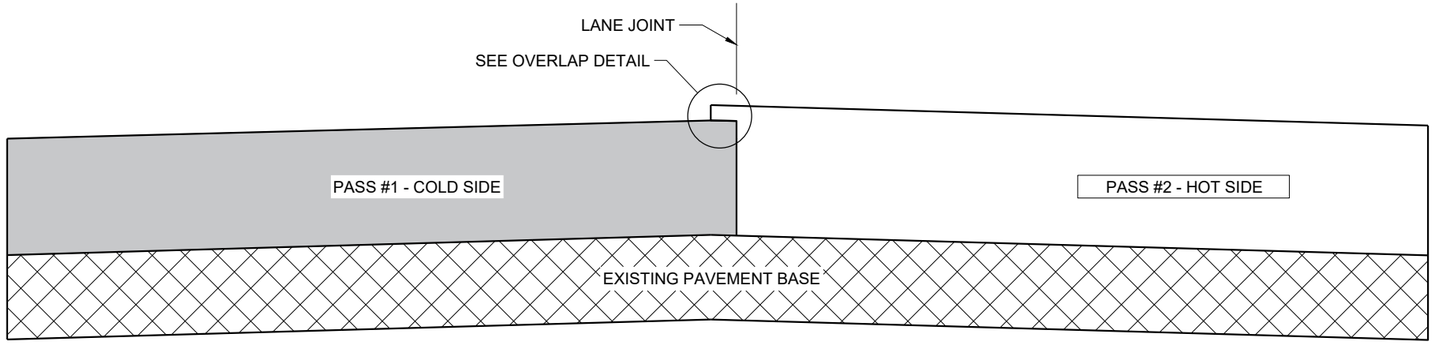
S.D.D. 13 B 1-10



TYPICAL PAVEMENT CROSS SECTION
NOTCHED WEDGE JOINT



TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT



TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT (MILLED)

GENERAL NOTES

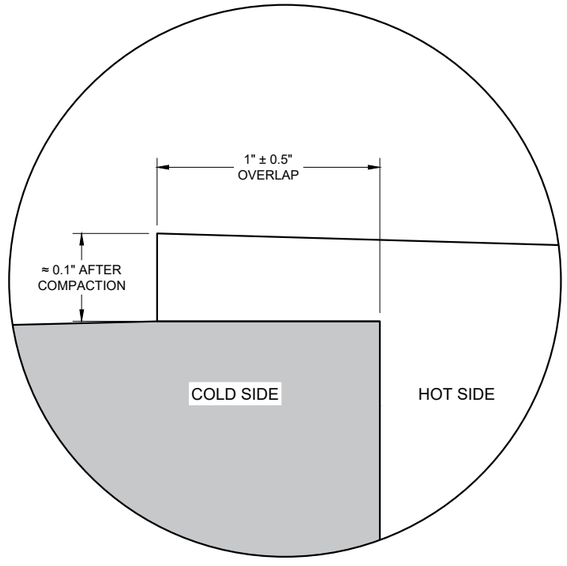
IN ADDITION TO THE DETAILS PROVIDED IN THIS DRAWING, CONFORM TO STANDARD SPECIFICATION 450.3.2.8 FOR WHEN A NOTCHED WEDGE JOINT IS REQUIRED AND FOR GENERAL JOINT CONSTRUCTION REQUIREMENTS.

FOR ALL LONGITUDINAL JOINTS, ENSURE THE PAVER SCREED OVERLAPS THE PREVIOUSLY PLACED PAVEMENT BY 1" ± 0.5" AND THE HOT SIDE OF THE JOINT REMAINS HIGHER THAN THE COLD SIDE BY APPROXIMATELY 0.1" AFTER FINAL COMPACTION. (IT WILL BE FLUSH WHEN PAVING IN ECHELON.)

ONLY REMOVE THE LONGITUDINAL NOTCHED WEDGE JOINT FOR SMA PAVEMENT OR AS DIRECTED BY THE ENGINEER TO ADDRESS SPECIFIC LENGTHS OF JOINT DAMAGED BY TRAFFIC.

WHEN MILLING BACK OR REMOVING ANY LONGITUDINAL JOINT, LIMIT THE MATERIAL REMOVED TO 2" FROM THE TOP NOTCH OR FROM THE VERTICAL JOINT EDGE ON THE COLD SIDE OF THE JOINT.

USE LONGITUDINAL MILLED JOINT AS PLANS SHOW OR THE AS THE ENGINEER DIRECTS.



OVERLAP DETAIL (TYPICAL)

6

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SDD 13C19 - 03

SDD 13C19 - 03

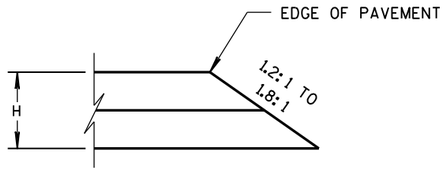
HMA LONGITUDINAL JOINTS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2020
DATE

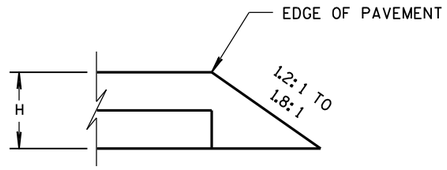
/S/ Steven Hefel
HMA PAVEMENT ENGIN

FHWA



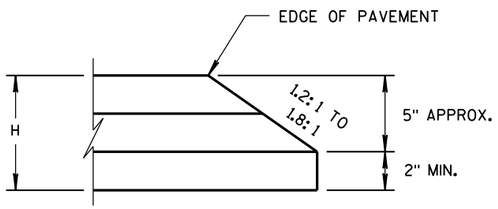
CONSTRUCTED WITH FINAL TWO LAYERS

FOR H 5" OR LESS



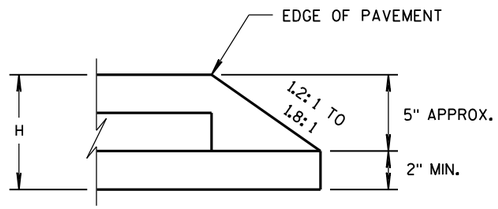
CONSTRUCTED WITH FINAL LAYER

FOR H 5" OR LESS



CONSTRUCTED WITH FINAL TWO LAYERS

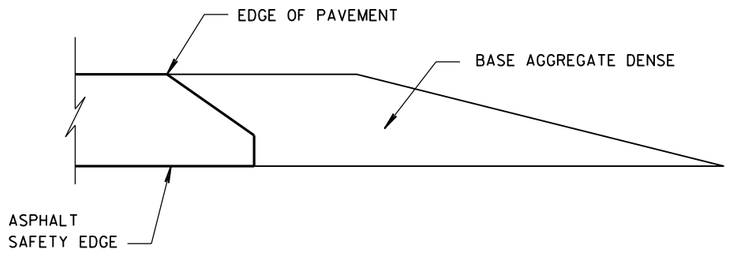
FOR H GREATER THAN 5"



CONSTRUCTED WITH FINAL LAYER

FOR H GREATER THAN 5"

HMA PAVEMENT AND HMA OVERLAYS



FINISHED SHOULDER AGGREGATE PLACEMENT

6

6

S.D.D. 14 B 29-1

S.D.D. 14 B 29-1

SAFETY EDGE SM

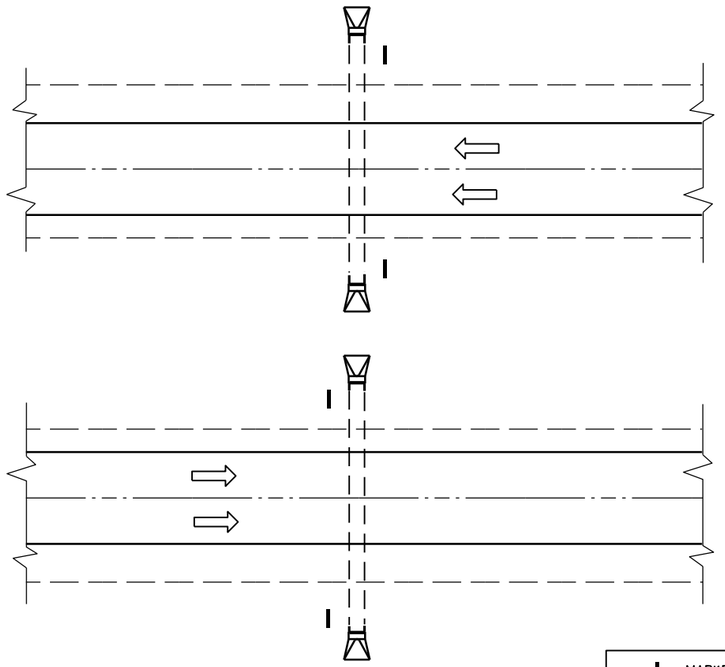
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

11/30/2012
DATE

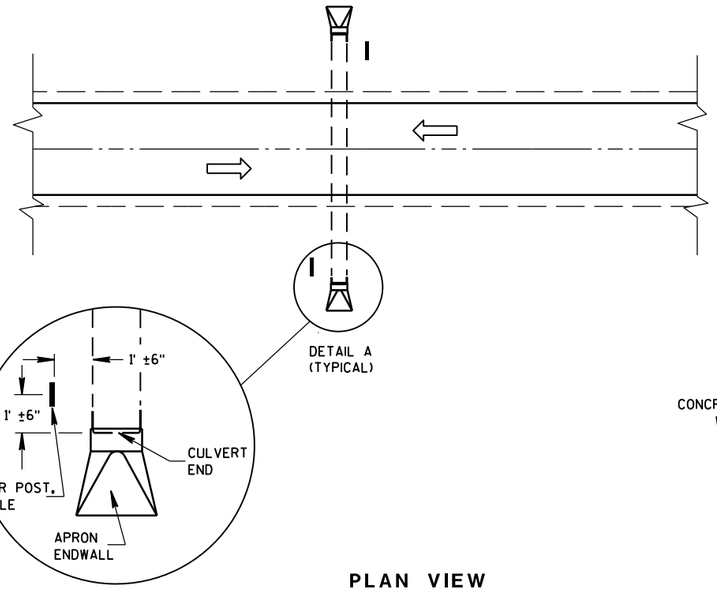
/S/ Jerry H. Zoda
ROADWAY STANDARDS DEVELOPMENT ENGINEER

FHWA



PLAN VIEW

DIVIDED HIGHWAY



DETAIL A
(TYPICAL)

PLAN VIEW

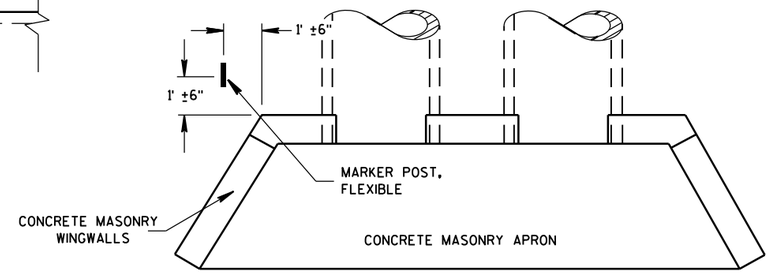
UNDIVIDED HIGHWAY

MARKER POST, FLEXIBLE

DIRECTION OF TRAFFIC FLOW

GENERAL NOTES

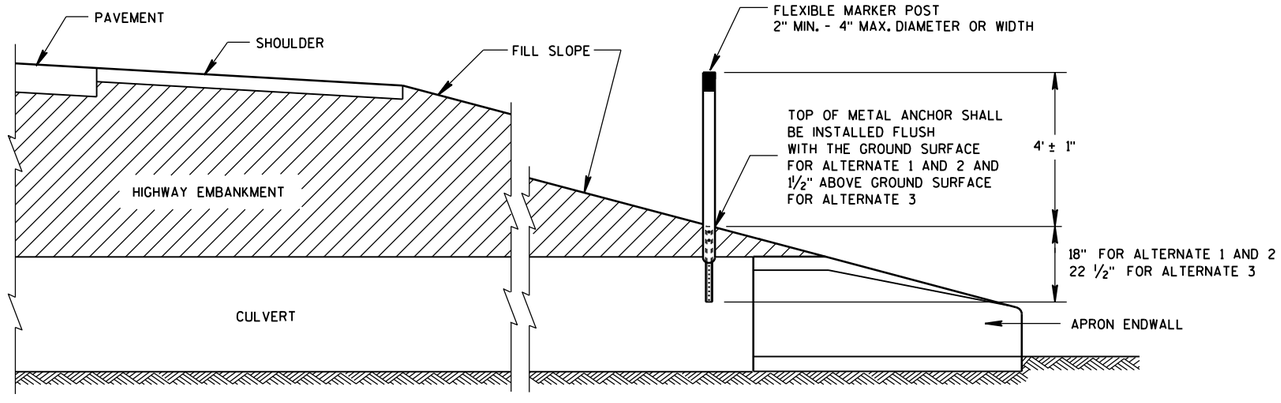
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.



PLAN VIEW

CONCRETE MASONRY ENDWALLS FOR CULVERT PIPE AND PIPE ARCH

FLEXIBLE MARKER POST LOCATION



CROSS SECTION

FLEXIBLE MARKER POST

FLEXIBLE MARKER POST FOR CULVERT END

STATE OF WISCONSIN
DEPARTMENT OF TRANSPOI

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S.D.D. 15 A 3-2a

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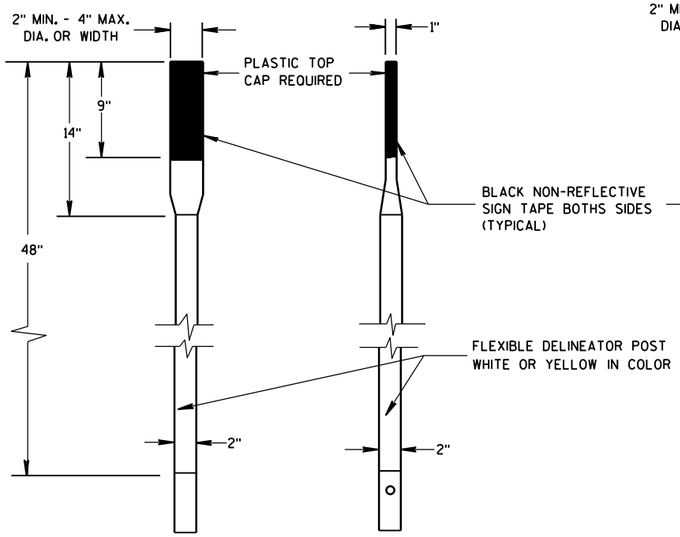
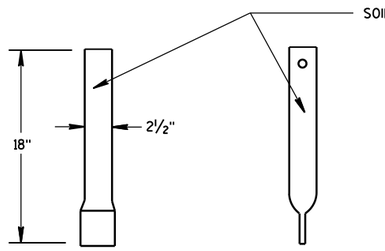
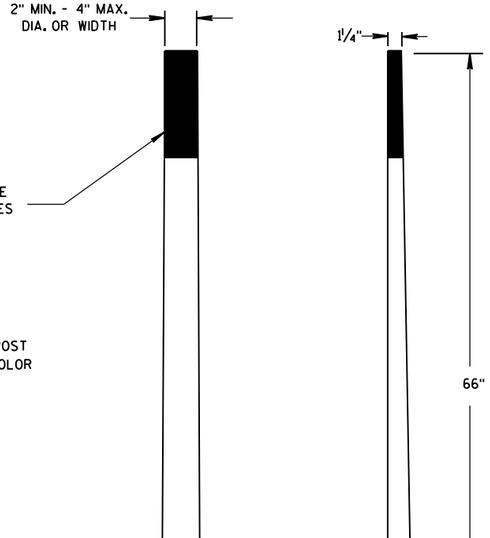
S.D.D. 15 A 3-2a





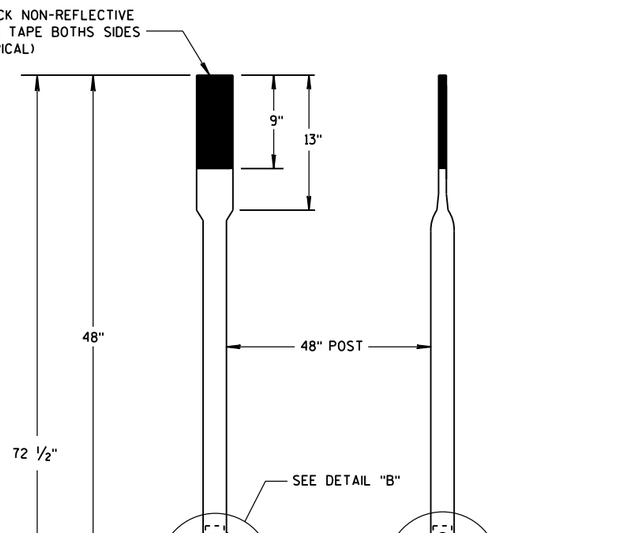
FRONT VIEW SIDE VIEW

ALTERNATE 1



FRONT VIEW SIDE VIEW

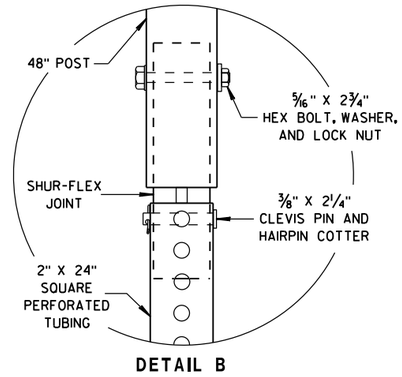
ALTERNATE 2



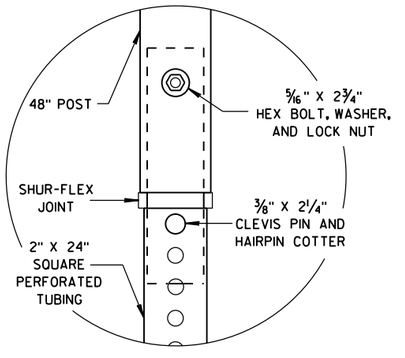
FRONT VIEW SIDE VIEW

ALTERNATE 3

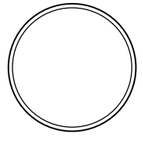
FLEXIBLE MARKER POSTS



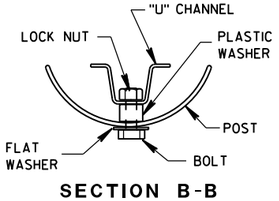
DETAIL B



DETAIL C



SECTION A-A



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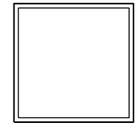
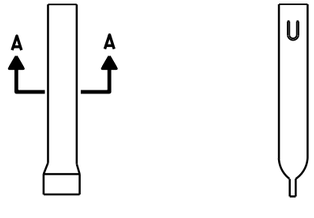
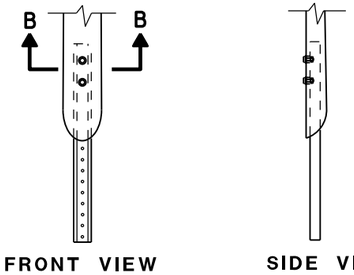


SECTION C-C



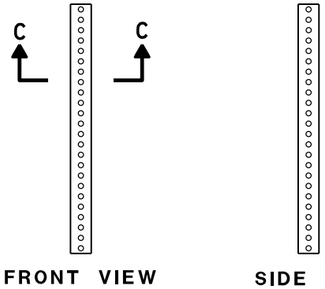
FRONT VIEW SIDE VIEW

ALTERNATE 1



FRONT VIEW SIDE VIEW

ALTERNATE 2



FRONT VIEW SIDE VIEW

ALTERNATE 3

FLEXIBLE MARKER POST ANCHORS

FLEXIBLE MARKER POST FOR CULVERT END

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
10/1/2012 — DATE
/S/ Travis Feltes — STATE TRAFFIC ENGINEER
FHWA

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S.D.D. 15 A 3-2b

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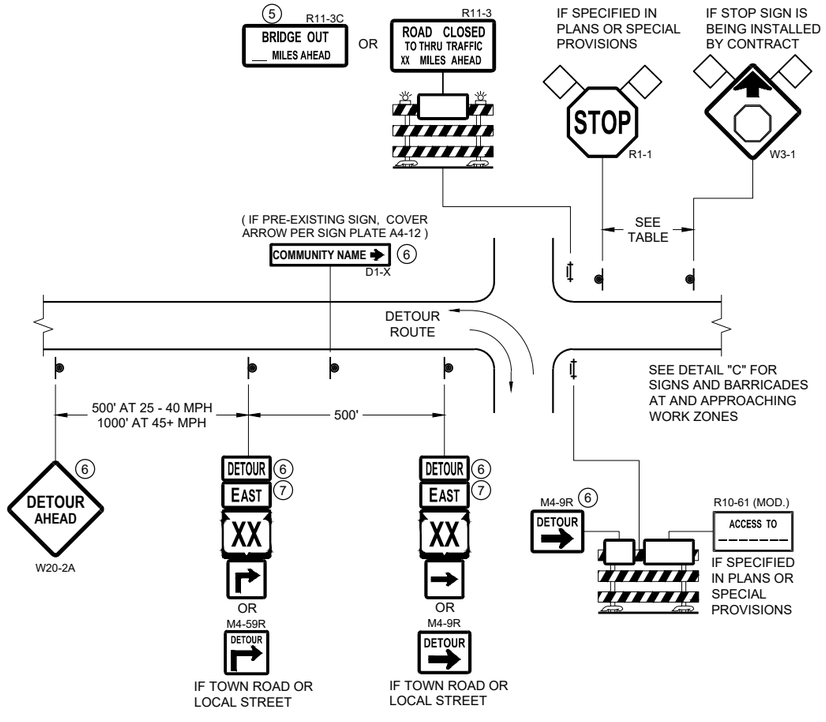
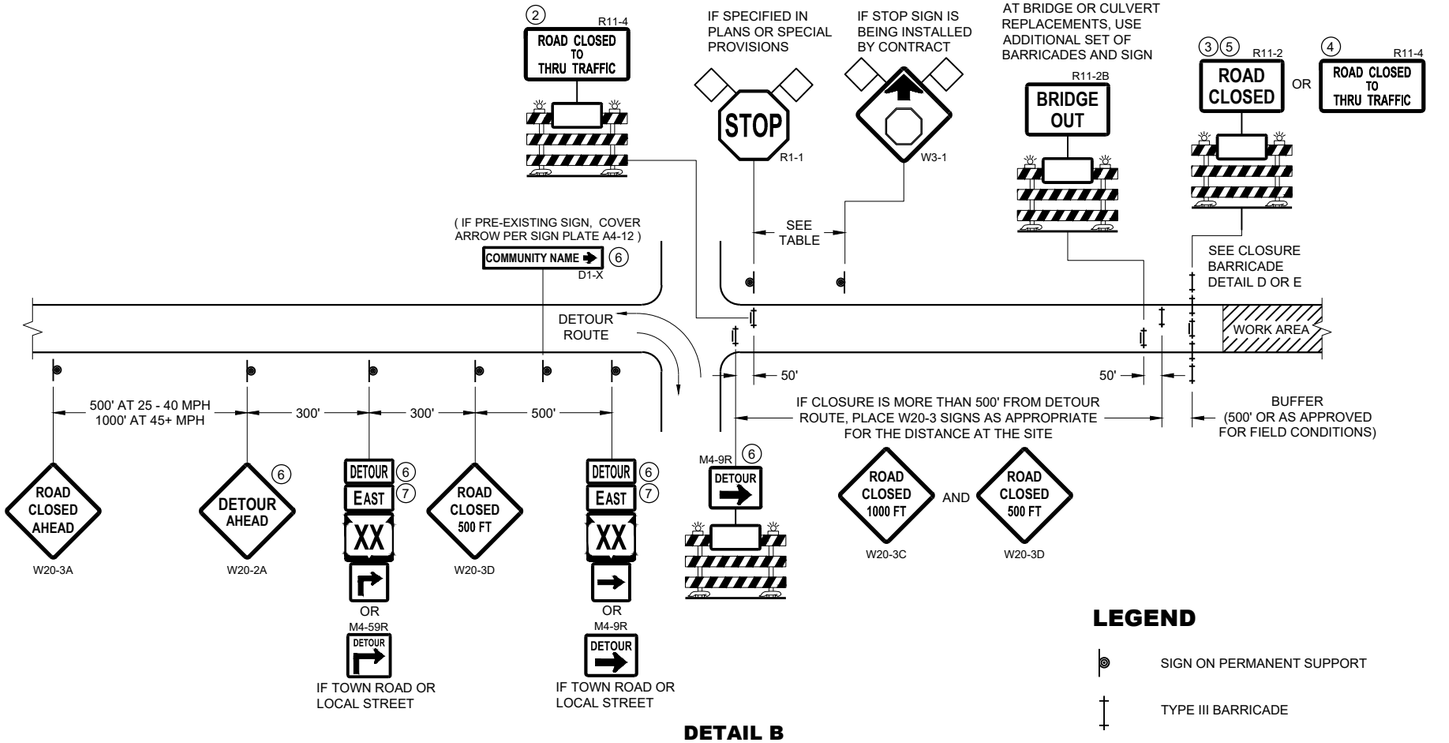
S.D.D. 15 A 3-2b



DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR

WORK ZONE GREATER THAN OR EQUAL TO ½ MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)



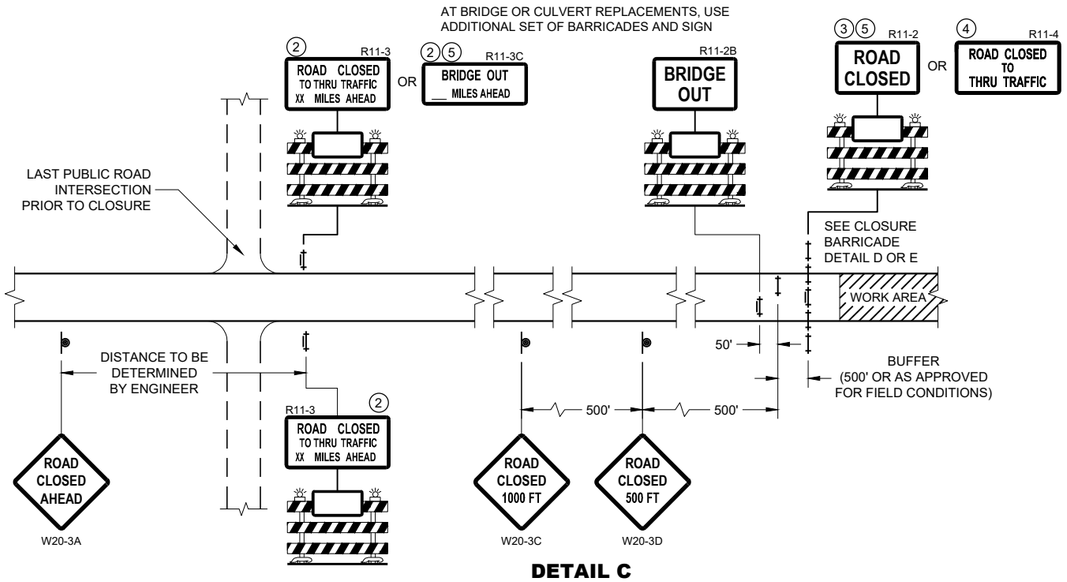
DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR

WORK ZONE LESS THAN ½ MILE FROM DETOUR ROUTE (1000 FEET IF URBAN)

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
- FLAGS, 16" X 16" MIN. (ORANGE)

DETOUR M4 - 8
EAST M3 - X
XX M1 - 4 OR XX M1 - 6 OR COUNTY X M1 - 5A
M05 - 1 OR M06 - 1



DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

SEE SDD 15C2-SHEET "b"
FOR GENERAL NOTES
AND FOOTNOTES ① THROUGH ⑦

BARRICADES AND SIGNS FOR MAINLINE CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020
DATE
/S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

SDD 15C02 - 08a

SDD 15C02 - 08a

THIS DRAWING PROVIDES GENERAL GUIDANCE ON TYPICAL DETOUR SIGN LAYOUT AND SPACING. SEE PROJECT DETOUR SIGNING SHEETS FOR SPECIFIC DETAILS FOR EACH PROJECT.

LEGEND

- SIGN ON PERMANENT SUPPORT
- WORK AREA
- DETOUR — M4 - 8
- EAST — M3 - X
- XX — M1 - 4 OR XX — M1 - 6 OR COUNTY X — M1 - 5A
- M05 - 1 OR M06 - 1 OR M06 - 1

GENERAL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. MODIFY EXISTING SIGNS WHERE POSSIBLE.

THE SPACING BETWEEN TRAFFIC CONTROL AND DETOUR SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

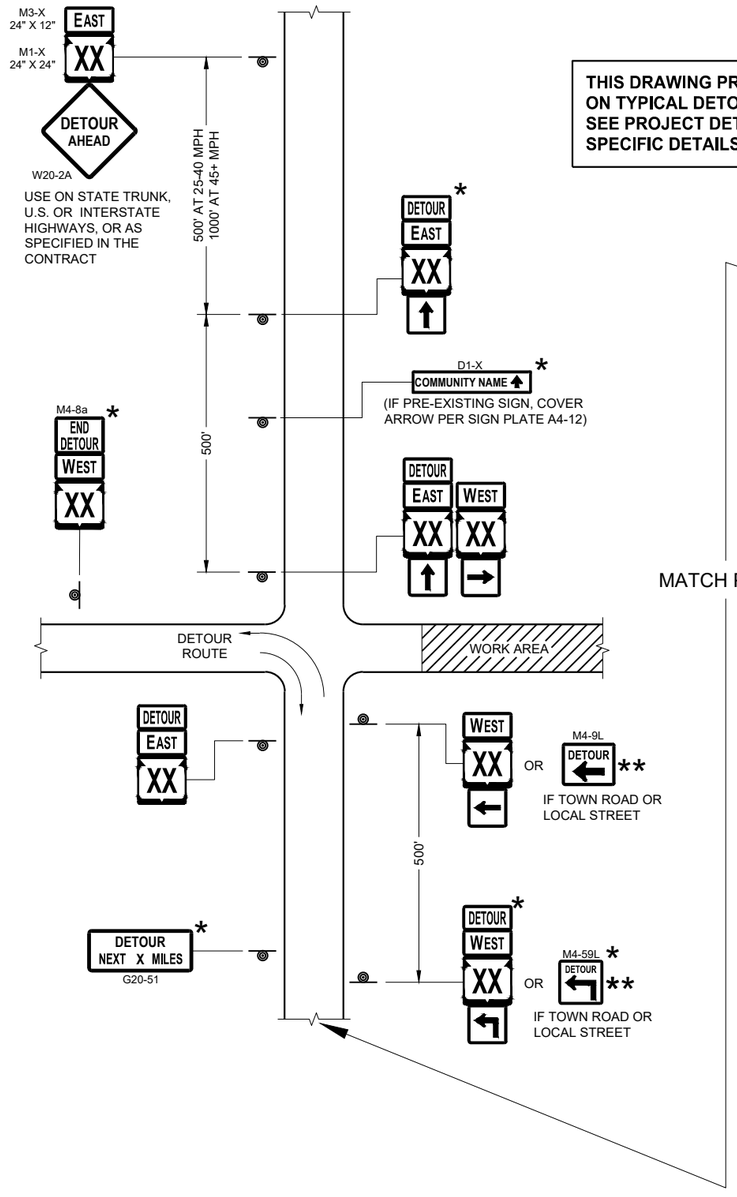
"MO" SIGNS ARE THE SAME AS "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

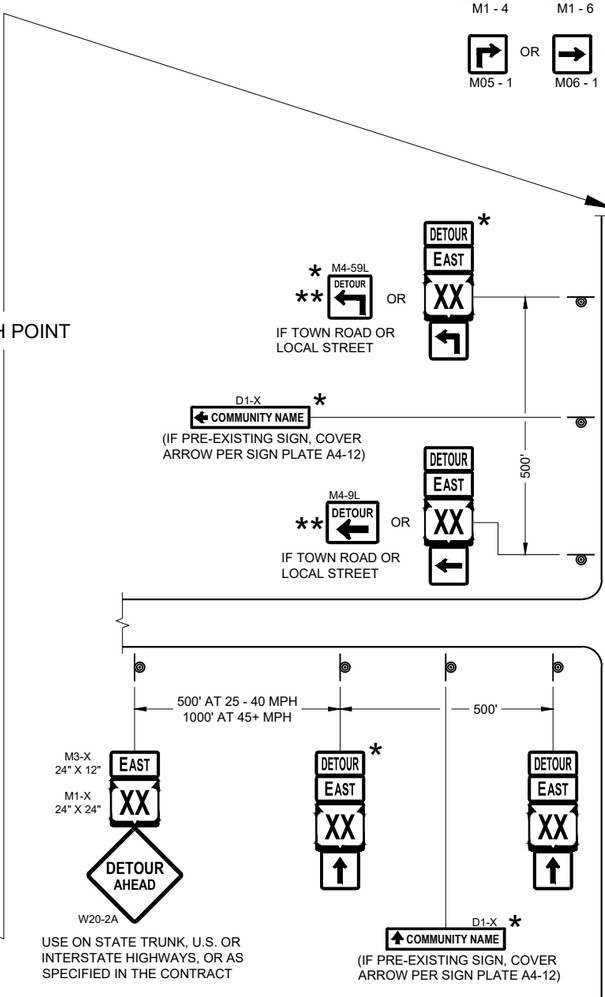
SIGN SIZES SHALL BE AS FOLLOWS:

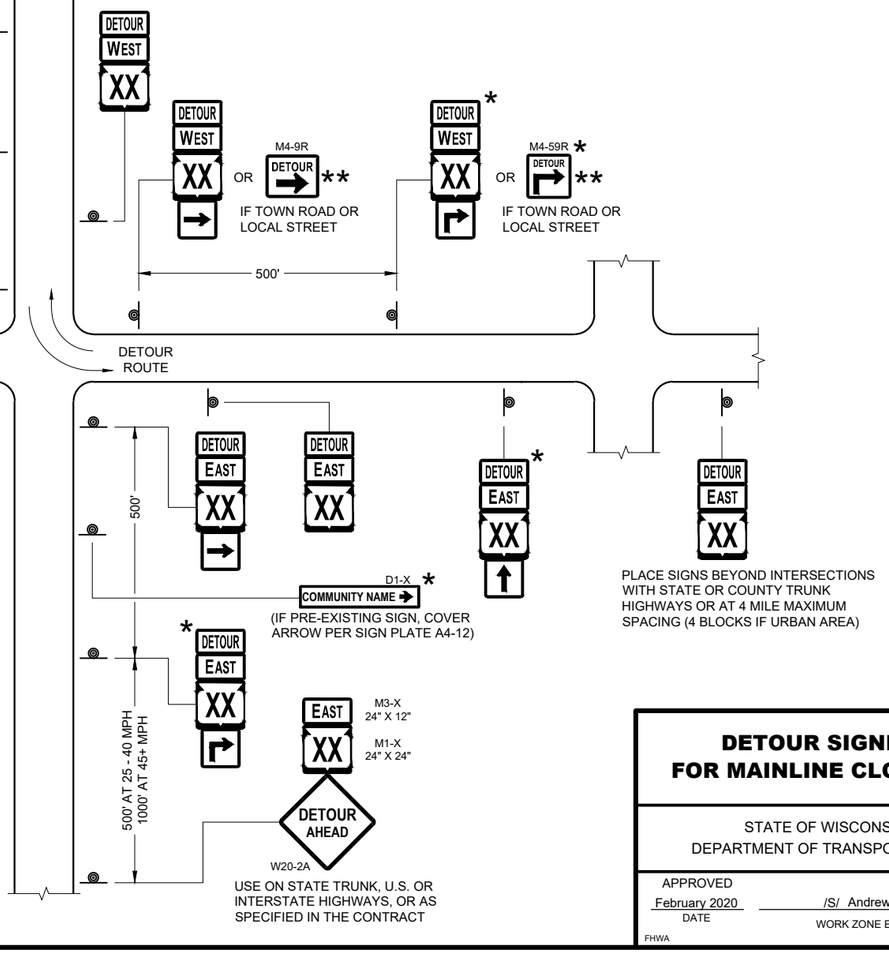
- M3-X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4-8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1-4, M1-5A AND M1-6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- M05-1 AND M06-1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- M4-9 AND M4-59 SHALL BE 30" X 24"
- M4-8a SHALL BE 24" X 18"
- G20-51 SHALL BE 60" X 24"
- W20-2A SHALL BE 48" X 48"
- D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.

* OPTIONAL SIGNS. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS.

** FOR A TOWN ROAD OR LOCAL STREET DETOURED ONTO A STATE TRUNK HIGHWAY, PLACE A ROAD NAME PLAQUE ABOVE THE M4-9 SIGN AS SPECIFIED IN THE CONTRACT.







SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS AND DETAIL A OR B ON SDD SHEET 15C02 - SHEET "a"

DETAIL F
DETOUR SIGNING

DETOUR SIGNING FOR MAINLINE CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

SDD 15C02 - 08c

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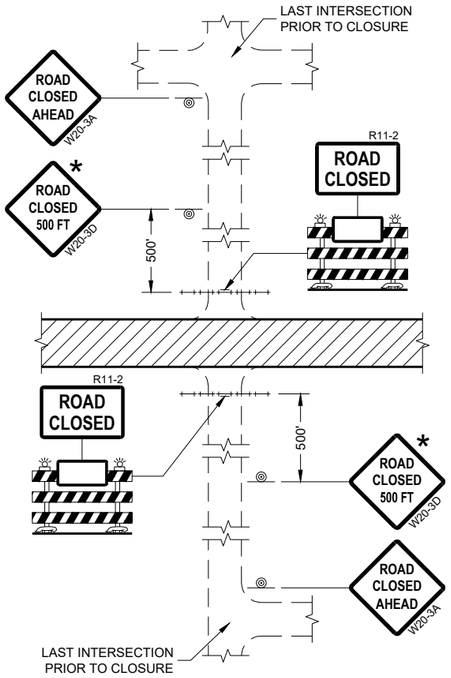
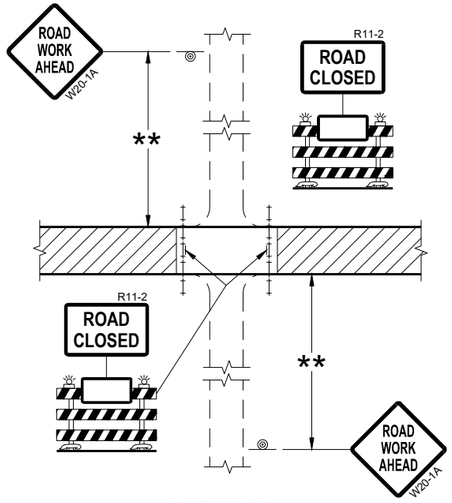
SDD 15C02 - 08c



DETAIL 1
(NO ACCESS TO PROJECT)



DETAIL 2
(PUBLIC CROSS-TRAFFIC MAINTAINED.
NO ACCESS TO PROJECT)

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE (500 FEET DESIRABLE) TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

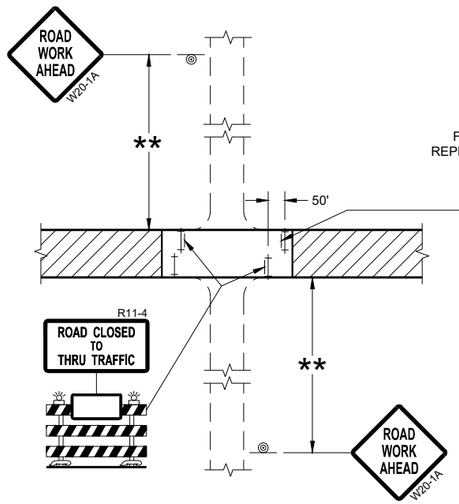
TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

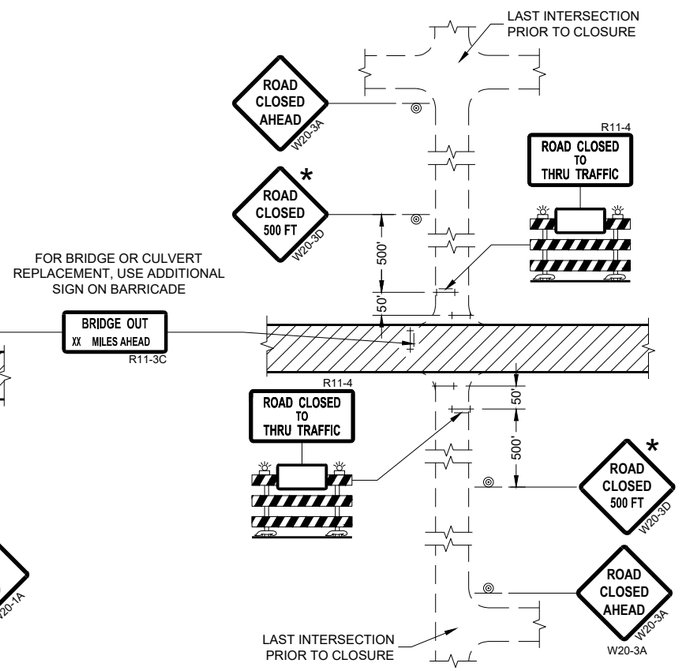
THE R11-2, R11-3, AND R11-4 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
R11-2 SHALL BE 48" X 30".
R11-4 AND R11-3 SHALL BE 60" X 30".

* OMIT THE "ROAD CLOSED 500 FT." SIGN IF THE LAST INTERSECTION IS 500 FEET OR LESS FROM THE WORK ZONE.

** 500' MAX. OR AT LAST INTERSECTION, WHICHEVER IS CLOSEST.





DETAIL 3
(PUBLIC CROSS-TRAFFIC MAINTAINED.
CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)

DETAIL 4
(CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA

BARRICADES AND SIGNS FOR SIDEROAD CLOSURES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
July 2018
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEE

FHWA

6

SDD 15C03 - 05

SDD 15C03 - 05

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

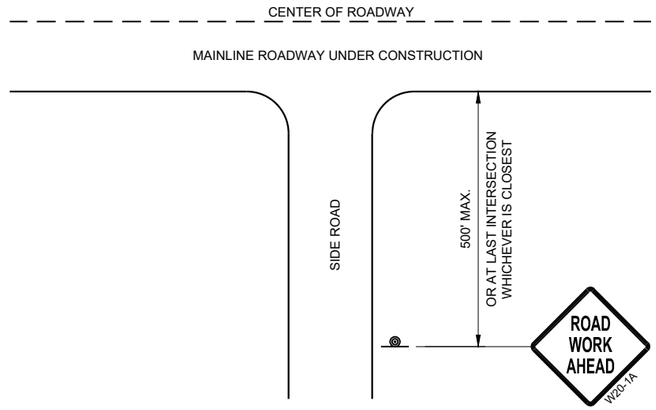
ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

* OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS

** PLACE AN ADDITIONAL W20-1A "ROAD WORK AHEAD" SIGN IF WORK AREA WITHIN THE PROJECT IS SEPARATED BY MORE THAN 2 MILES FROM PREVIOUS WORK AREA.



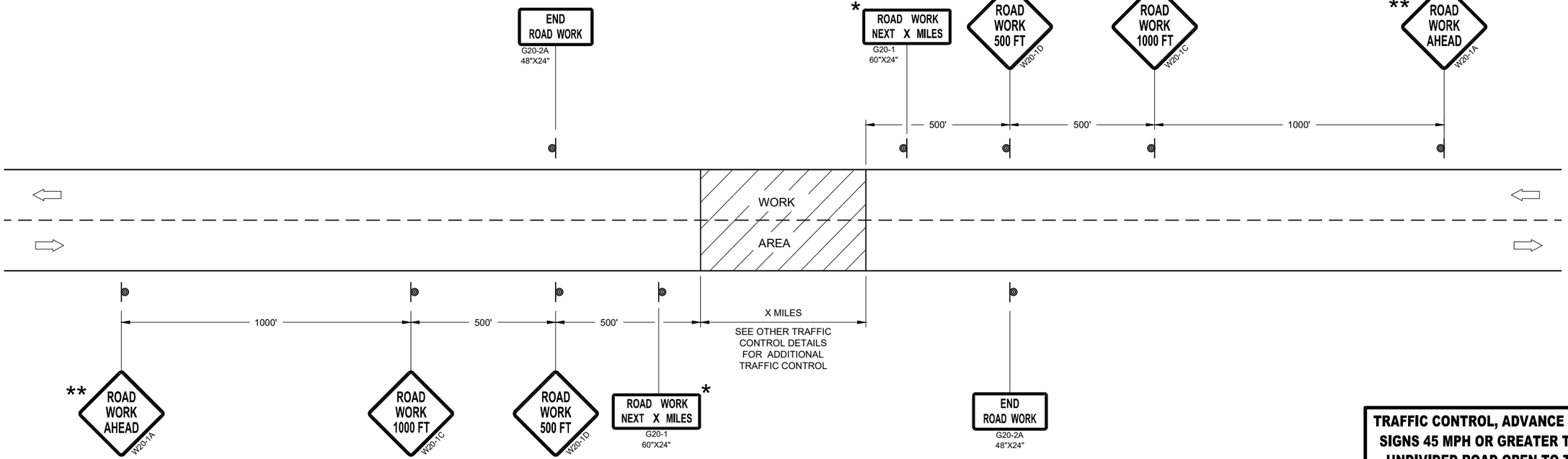
TYPICAL SIDE ROAD APPROACH WARNING SIGN DETAIL

LEGEND

SIGN ON PERMANENT SUPPORT

DIRECTION OF TRAFFIC

WORK AREA



TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45MPH OR GREATER

TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 MPH OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
July 2018
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEE

FHWA

6

SDD 15C04 - 05

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

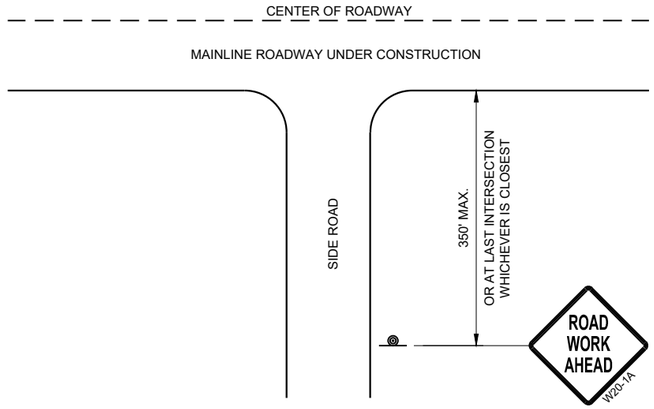
THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS, 36"X36" SIGNS MAY BE USED INSTEAD OF 48" X 48" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

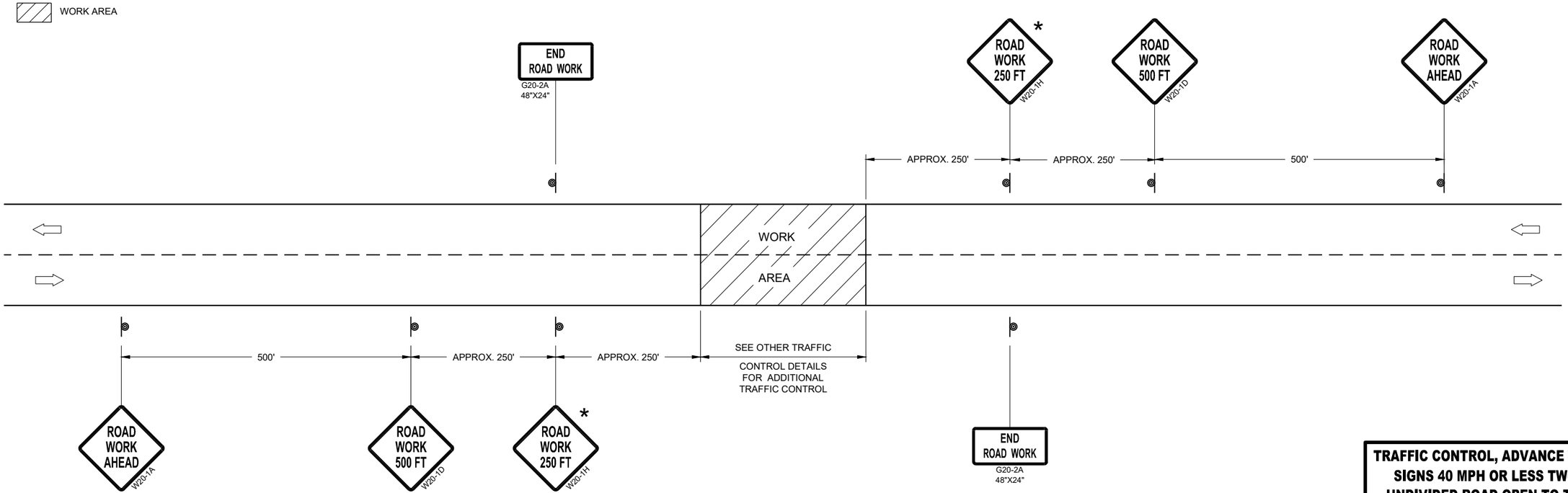
* THE THIRD W20-1 SIGN IS REQUIRED ONLY IF THERE IS AN INTERSECTION BETWEEN THE "ROAD WORK 500 FEET" SIGN AND THE WORK ZONE. ADJUST THE PLACEMENT OF THIS SIGN BASED ON INTERSECTION LOCATION AND OTHER FIELD CONDITIONS.



TYPICAL SIDE ROAD APPROACH WARNING SIGN DETAIL

LEGEND

- SIGN ON PERMANENT SUPPORT
- DIRECTION OF TRAFFIC
- WORK AREA



TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40MPH OR LESS

TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 MPH OR LESS TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
July 2018
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEE

FHWA

6

6

SDD 15C05 - 05

SDD 15C05 - 05

GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

(1) LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING

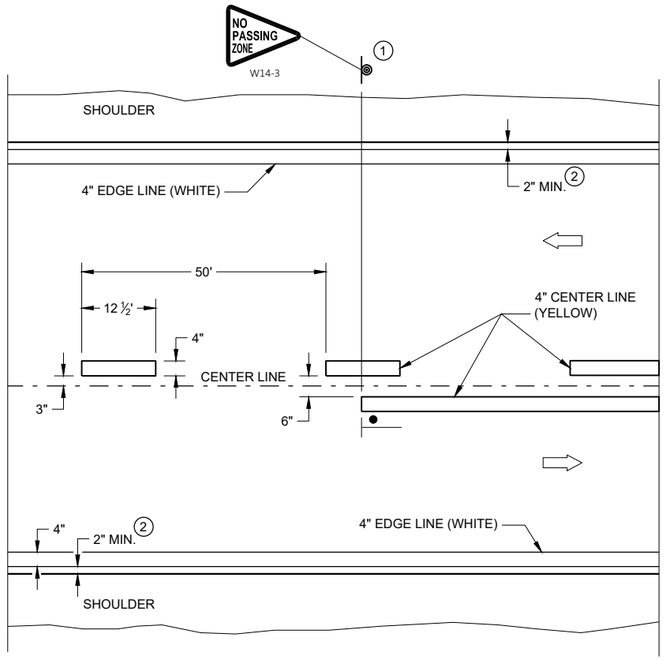
(2) MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

LEGEND

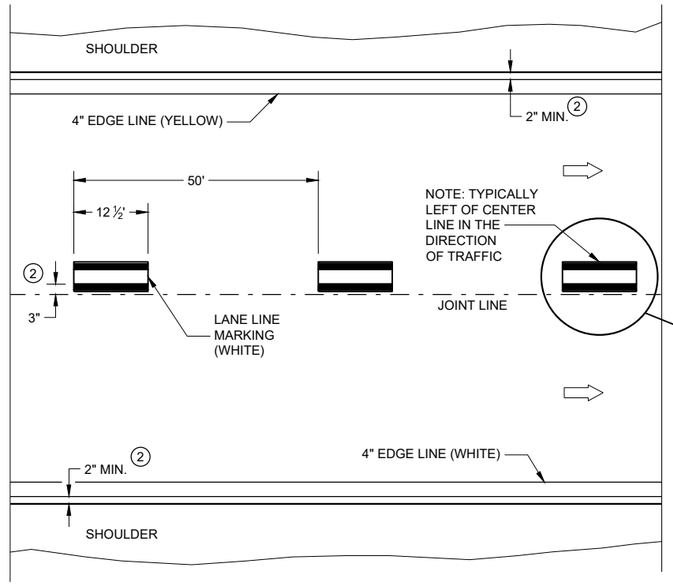
"T" MARKING

SIGN ON PERMANENT SUPPORT

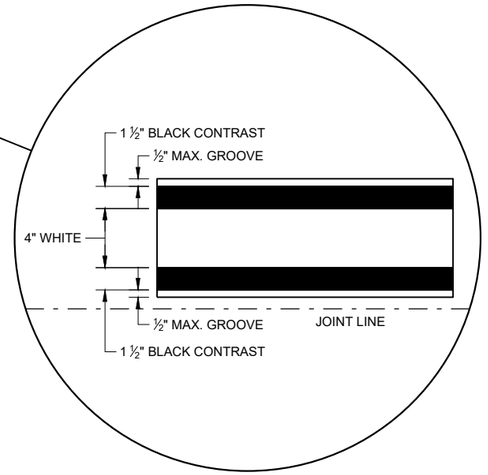
DIRECTION OF TRAFFIC



TWO WAY TRAFFIC



ONE WAY TRAFFIC



PERMANENT PAVEMENT MARKING

PERMANENT LONGITUDINAL PAVEMENT MARKINGS

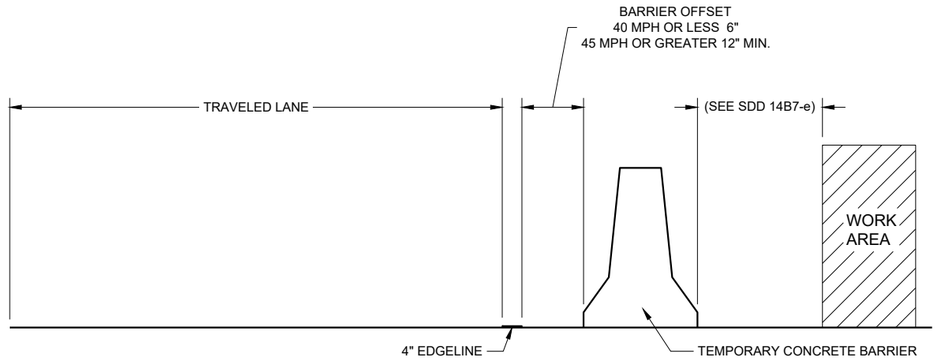
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2022 — DATE
/S/ Jeannie Silver — STATEWIDE SIGNING AND M ENGINEER

FHWA

SDD 15C08 - 21a

SDD 15C08 - 21a



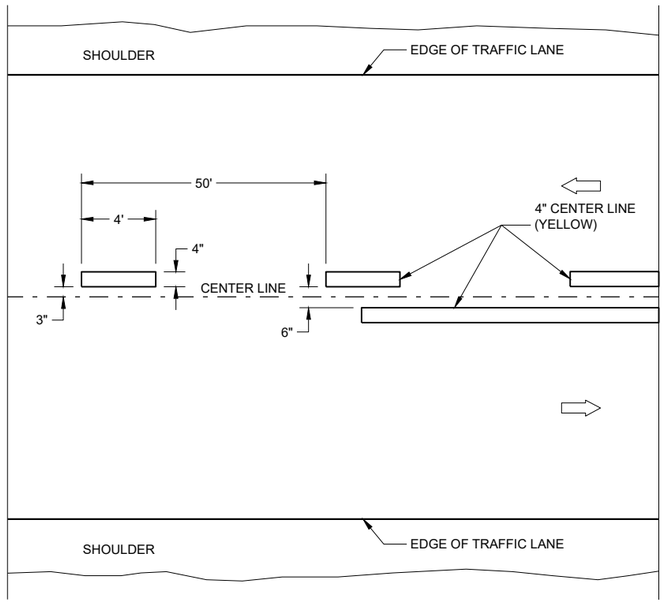
TEMPORARY BARRIER OFFSET FROM EDGELINE

GENERAL NOTES

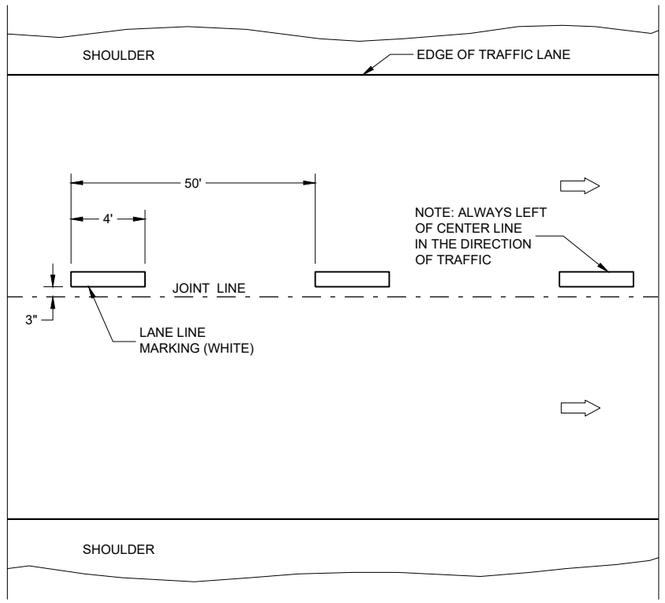
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

LEGEND

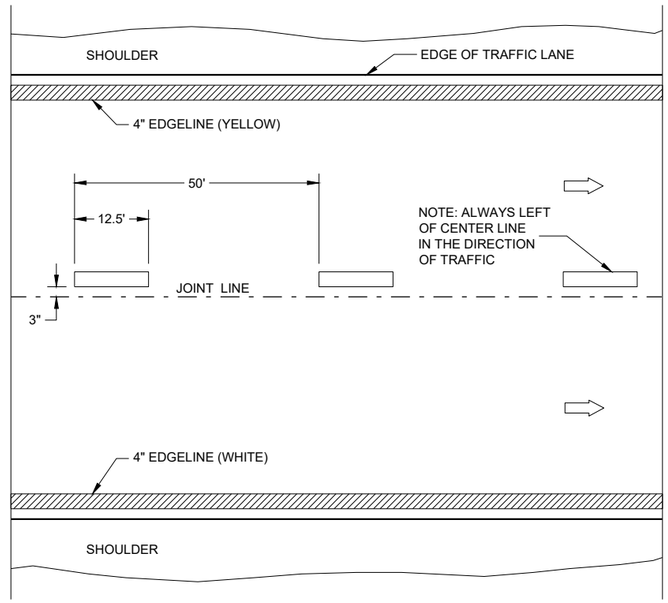
DIRECTION OF TRAFFIC



TWO WAY TRAFFIC



ONE WAY TRAFFIC



FREEWAYS AND EXPRESSWAYS

TEMPORARY PAVEMENT MARKING

6

SDD 15C08 - 21b

TEMPORARY LONGITUDINAL PAVEMENT MARKING	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2022 DATE	/S/ Jeannie Silver STATEWIDE SIGNING AND M ENGINEER
FHWA	112

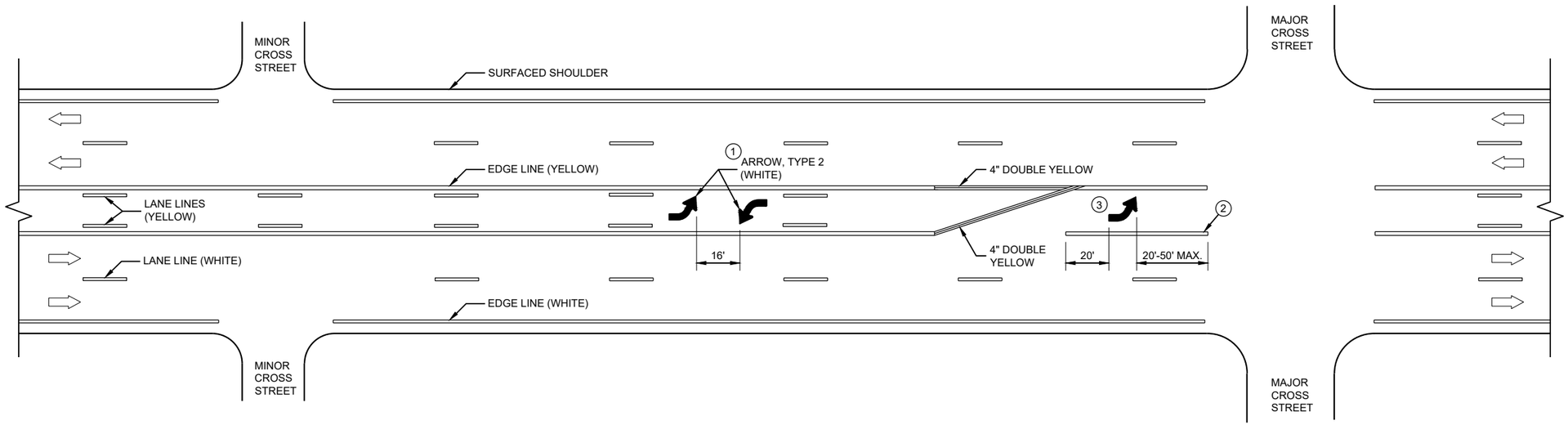
6

SDD 15C08 - 21b

GENERAL NOTES

1. A SET OF ARROWS IS REQUIRED EVERY 400 FEET OR NEAR INTERSECTIONS OR DRIVEWAYS WITH TURNING TRAFFIC.
2. 8" WHITE
3. TURN BAY LENGTH OF LESS THAN 48' DOES NOT REQUIRE PAVEMENT ARROWS OR TEXT.

DIRECTION OF TRAFFIC



TWO WAY LEFT TURN LANE

6

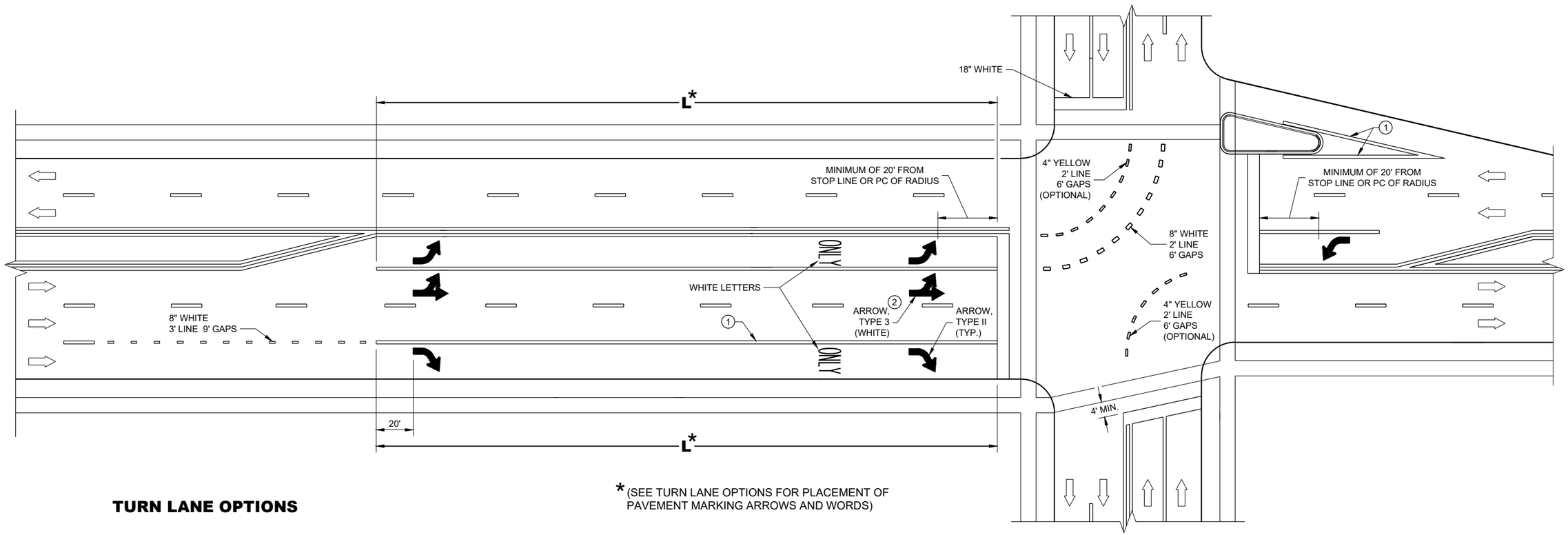
SDD 15C08 - 21c

PAVEMENT MARKING (TURN LANES)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

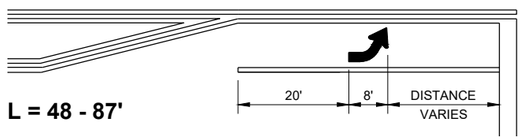
SDD 15C08 - 21c



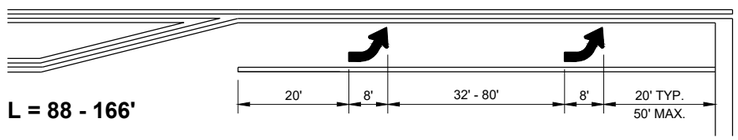
* (SEE TURN LANE OPTIONS FOR PLACEMENT OF PAVEMENT MARKING ARROWS AND WORDS)

TURN LANE OPTIONS

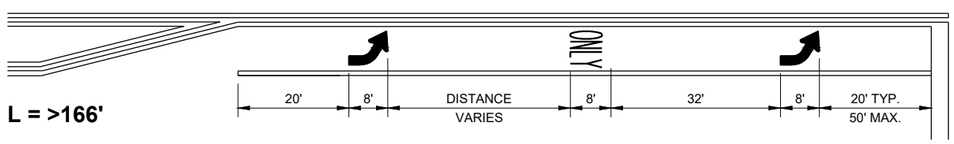
LENGTH OF TURN BAY (**L**) OF 0 - 47' DOES NOT REQUIRE PAVEMENT MARKING ARROWS OR WORDS



L = 48 - 87'



L = 88 - 166'



L = >166'

GENERAL NOTES

1. 8" WHITE
2. QUANTITY AND LOCATION OF TYPE 3 ARROWS ARE THE SAME AS THE TYPE II ARROWS IN THE ADJACENT TURN LANE. FOR TURN LANES WITH A PHYSICAL SEPARATION IN THE SAME DIRECTION OF TRAVEL, THE ARROWS AND "ONLY" MARKING MAY BE ELIMINATED.

DIRECTION OF TRAFFIC

L = LENGTH OF TURN BAY

PAVEMENT MARKING (TURN LANES)

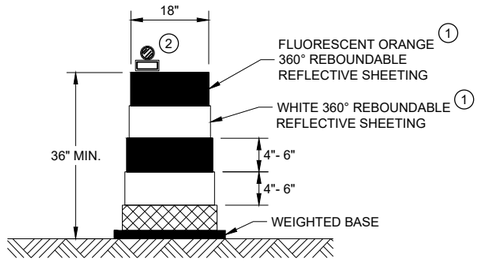
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

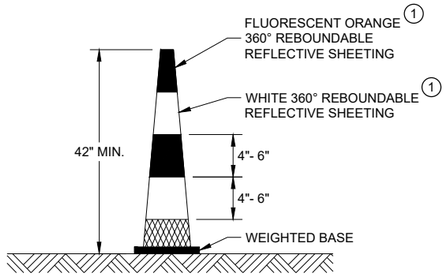
SDD 15C08 - 21d

GENERAL NOTES

1. REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
2. LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.

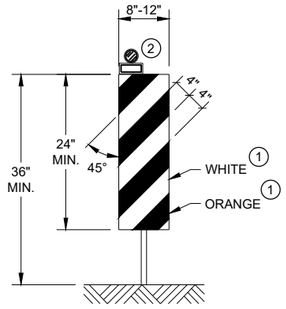


DRUM



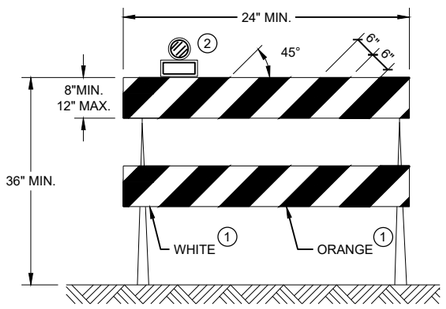
42" CONE

DO NOT USE IN TAPERS
½ SPACING OF DRUMS



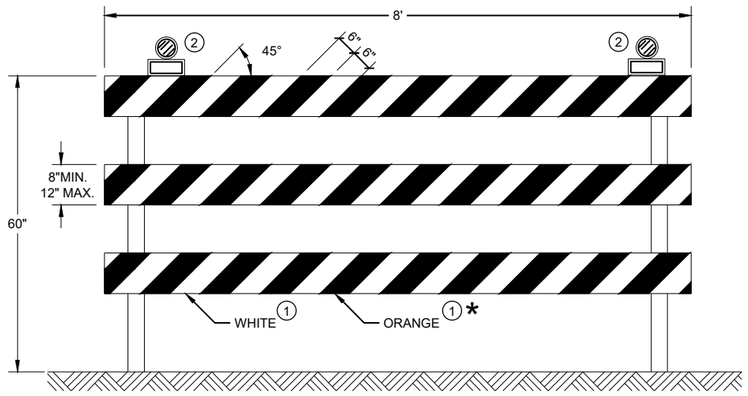
VERTICAL PANEL

THE STRIPES SHALL SLOPE DOWNWARD TO
THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE II BARRICADE

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES
MAY BE USED. ALL STRIPES SHALL SLOPE DOWNWARD
TO THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE III BARRICADE

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP
TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.

* IF USED FOR A PERMANENT APPLICATION USE RED SHEETING.

CHANNELIZING DEVICES
DRUMS, CONES, BARRICADES
AND VERTICAL PANELS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2021
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEE

FHWA

6

SDD15C11 - 09b

6

SDD15C11 - 09b

LEGEND

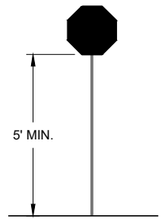
SIGN ON PORTABLE OR PERMANENT SUPPORT

TEMPORARY PORTABLE RUMBLE STRIP ARRAY

DIRECTION OF TRAFFIC

WORK AREA

FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF



STOP/SLOW PADDLE ON SUPPORT STAFF

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS, DEVICES, AND LOCATION OF ALL FLAGGERS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGING

FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT REMOVE TEMPORARY PORTABLE RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.

(1) FOR MOVING WORK OPERATIONS, POST ADDITIONAL W20-7A FLAGGER SIGNS AT APPROXIMATELY 3,500' INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.

(2) SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

WHEN THE DISTANCE BETWEEN FLAGGERS EXCEEDS 2 MILES, A PILOT CAR IS REQUIRED. WHEN CURVES REDUCE SIGHT DISTANCE BELOW 400', A PILOT CAR IS REQUIRED.

TEMPORARY PORTABLE RUMBLE STRIPS

UTILIZE TEMPORARY PORTABLE RUMBLE STRIPS ON ALL FLAGGING OPERATIONS.

(3) EACH TEMPORARY PORTABLE RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS PLACED TRANSVERSE ACROSS THE LANE AT THE LOCATIONS SHOWN. WITHIN EACH ARRAY, SPACING BETWEEN RUMBLE STRIPS SHALL BE 15 FEET ON CENTER

ONLY USE TEMPORARY PORTABLE RUMBLE STRIPS FROM THE APPROVED PRODUCTS LIST.

INSTALL TEMPORARY RUMBLE STRIPS PER MANUFACTURER'S RECOMMENDATIONS.

PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.

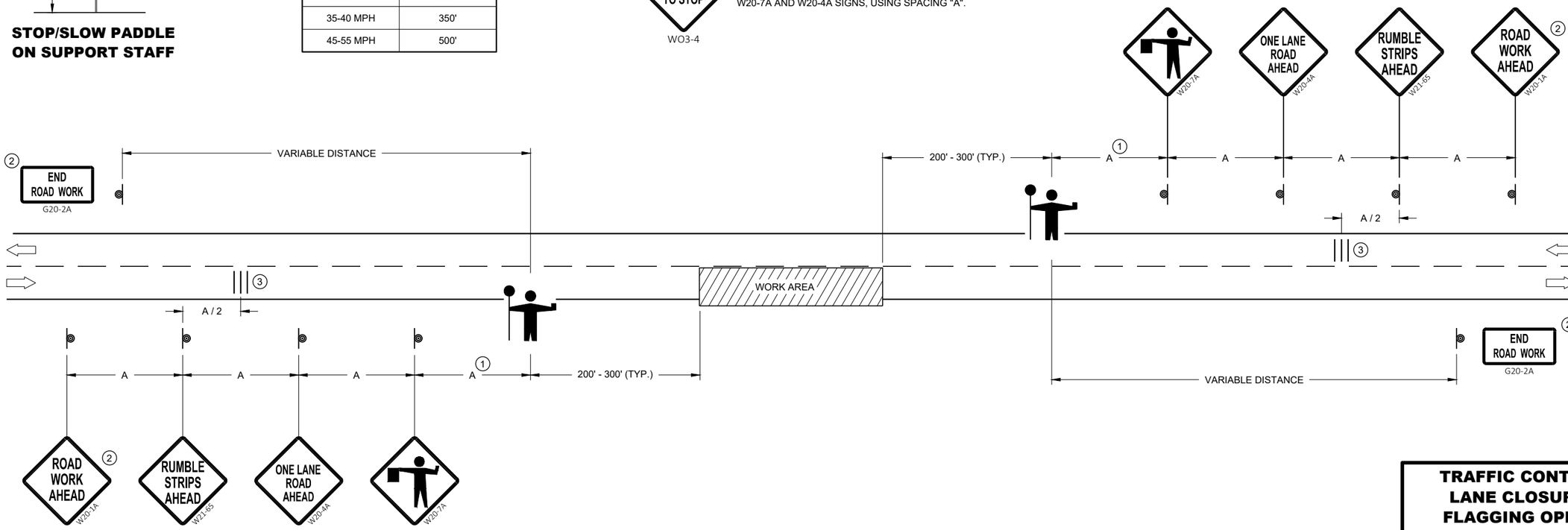
DO NOT INSTALL TEMPORARY PORTABLE RUMBLE STRIPS ON GRAVEL, MILLED SURFACES, OR ASPHALT THAT HAS BEEN PAVED LESS THAN 12 HOURS.

SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE

SPEED LIMIT	SPACING "A"
25-30 MPH	200'
35-40 MPH	350'
45-55 MPH	500'

WO3-4

USE OF WO3-4 SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7A AND W20-4A SIGNS, USING SPACING "A".



TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2022
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEE

FHWA

SDD 15C12 - 09a

6

SDD 15C12 - 09a

LEGEND

V1 LEAD VEHICLE

V2 MARKING VEHICLE

V3 SHADOW VEHICLE

TRUCK MOUNTED ATTENUATOR (TMA)

SIGN ON TEMPORARY SUPPORT

DIRECTION OF TRAFFIC

FLASHING ARROW PANEL (CAUTION)

GENERAL NOTES

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

ALL VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL OPERATING IN CAUTION MODE. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.

DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL AND HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.

THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS.

WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR LAY STATIONARY SIGNS AND SUPPORTS FLAT ON THE GRADE WITH UPRIGHTS ORIENTED PARALLEL TO AND DOWNSTREAM FROM TRAFFIC.

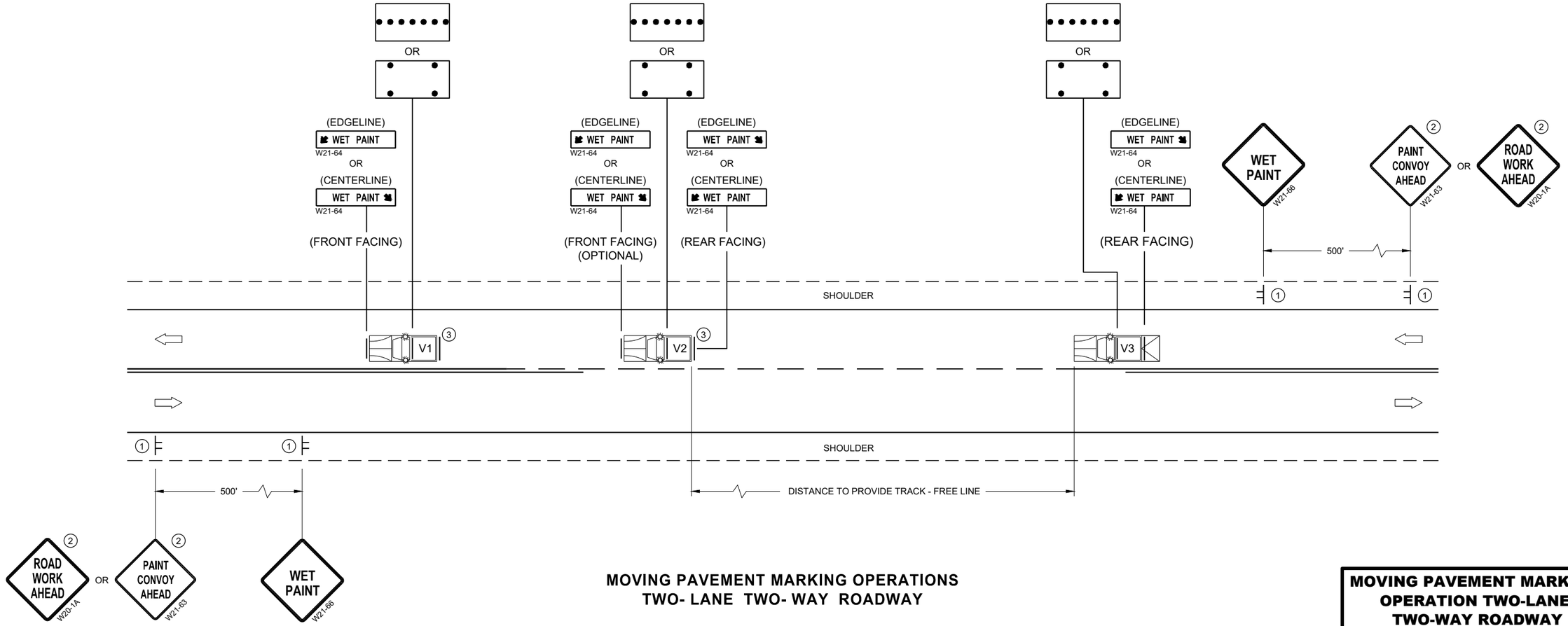
CONES SHOULD BE USED BETWEEN THE MARKING AND SHADOW VEHICLE AT 100 FOOT SPACING. CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.

CONES SHALL BE A MINIMUM OF 28" FOR WET PAVEMENT MARKING .

(1) SIGNS SHALL BE REPEATED APPROXIMATELY EVERY THREE MILES.

(2) IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1A OR W21-63 ARE NOT REQUIRED.

(3) V1 AND V2 CAN BE SWITCHED SO THAT THE MARKER IS THE LEAD VEHICLE.



MOVING PAVEMENT MARKING OPERATIONS
TWO- LANE TWO- WAY ROADWAY

MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2022
DATE

/S/ Andrew Heidtke
WORK ZONE ENGINEE

FHWA

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SDD 15C19 - 07a

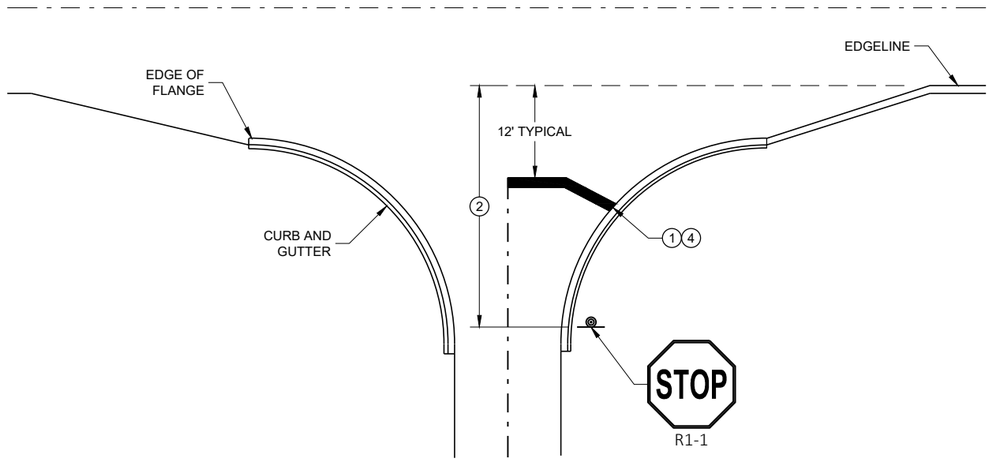
6

SDD 15C19 - 07a

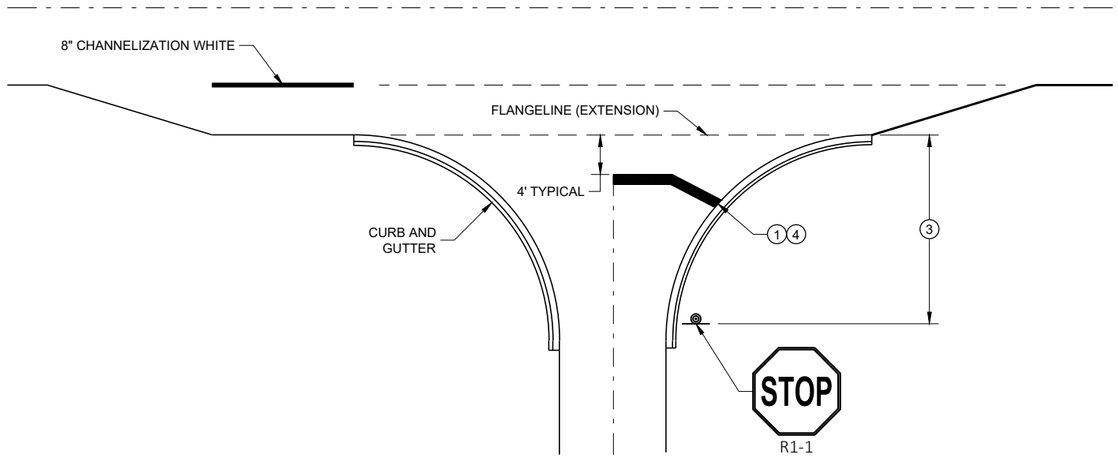
GENERAL NOTES

STOP SIGN SHALL BE PLACED A MINIMUM OF 6 FEET TO A MAXIMUM OF 50 FEET FROM THE EDGELINE LOCATION.

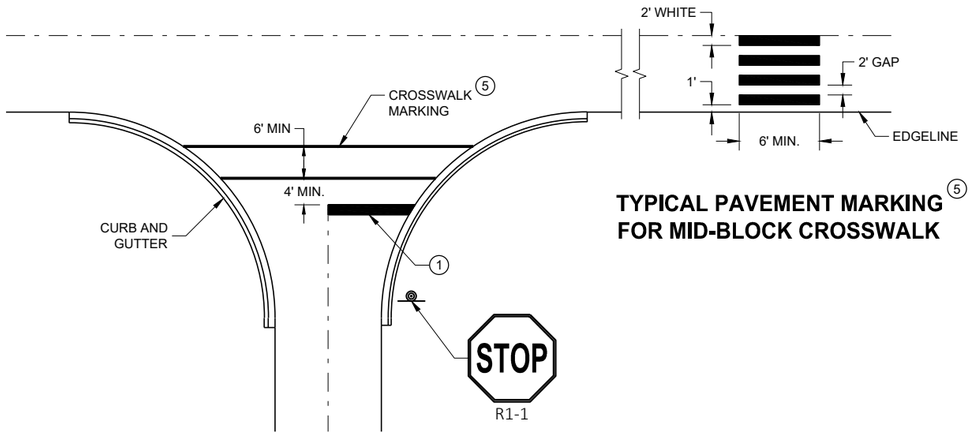
1. 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE REGION MARKING ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
2. NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGELINE.
3. NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGELINE EXTENSION.
4. MOVE CLOSER TO THE EDGE OF TRAVEL LINE AS NEEDED FOR VISIBILITY AND SIGHT LINES (NO CLOSER THAN 4 FEET).
5. LADDER BAR CROSSWALKS SHOULD ONLY BE USED FOR MID BLOCK CROSSINGS. USE 2 - 6" TRANSVERSE LINES INSTEAD.



TYPICAL STOP LINE PAVEMENT MARKING WITH CURB AND GUTTER

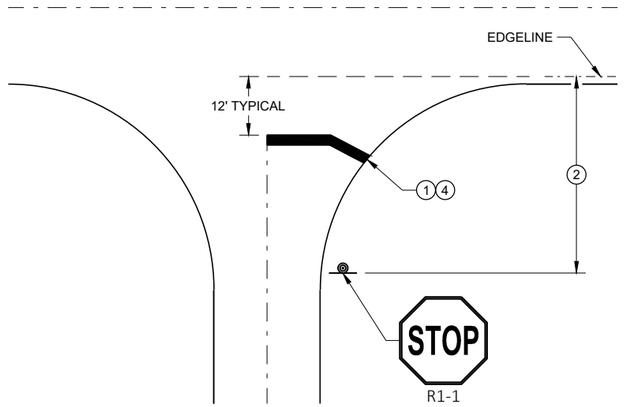


TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH RIGHT TURN LANE



TYPICAL PAVEMENT MARKING FOR MID-BLOCK CROSSWALK 5

TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH CROSSWALK MARKING



TYPICAL STOP LINE PAVEMENT MARKING WITHOUT CURB AND GUTTER

STOP LINE AND CROSSWALK PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2019
DATE

/S/ Matthew Rauch
STATE SIGNING AND MARKING ENGINEER

FHWA

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SDD 15C33 - 04

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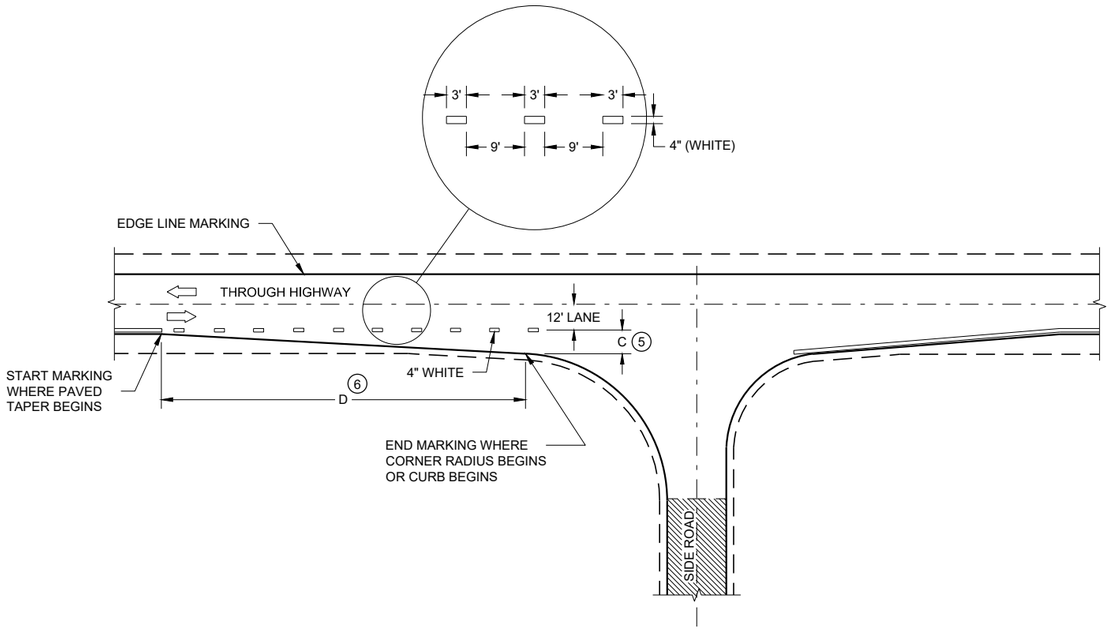
SDD 15C33 - 04



MINOR INTERSECTION

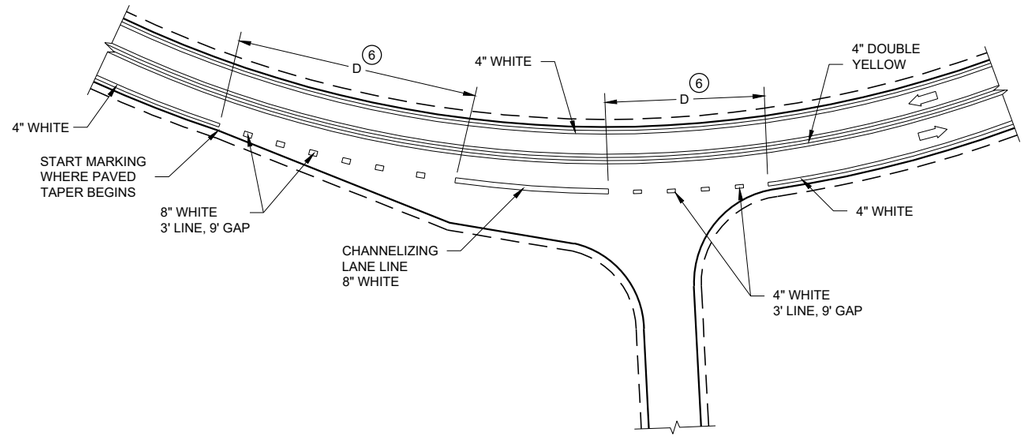
GENERAL NOTES

OMIT EDGE LINES THROUGH INTERSECTIONS. CONTINUE EDGE LINES THROUGH DRIVEWAYS.

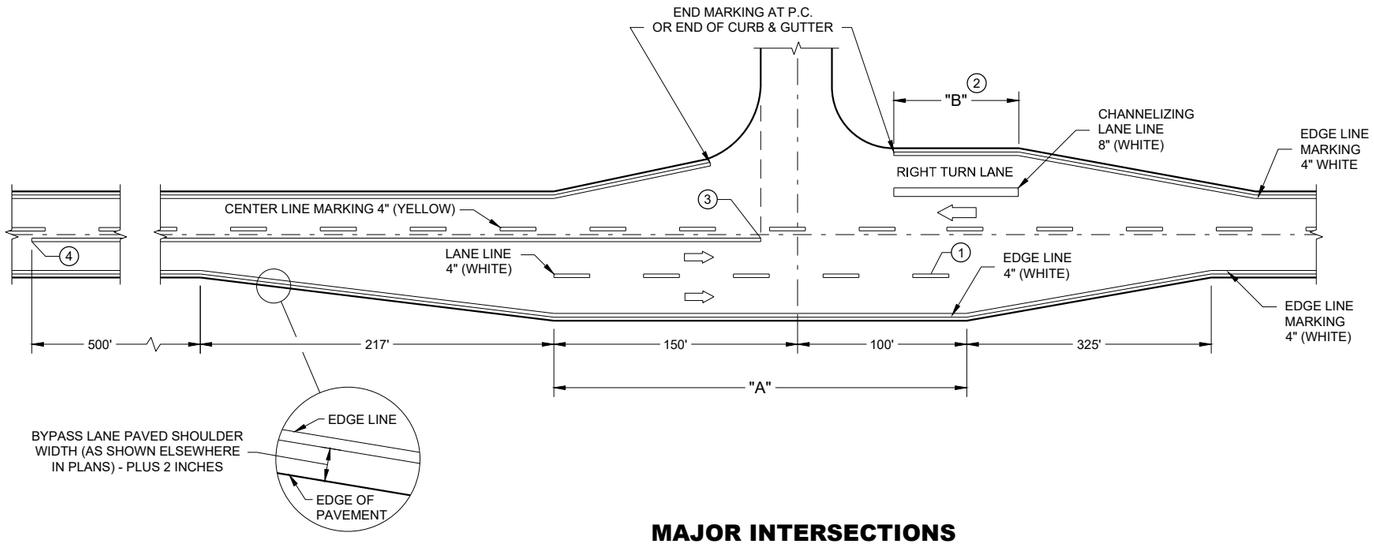
1. WHEN DISTANCE "A" IS LESS THAN 250 FEET, OMIT LANE LINE.
2. WHEN DISTANCE "B" IS LESS THAN 100 FEET, OMIT CHANNELIZING LANE LINE.
3. BARRIER LINE ENDS AT SIDE ROAD PAVEMENT / SURFACE EDGE EXTENSION.
4. BARRIER LINE STARTS 500 FEET PRIOR TO THE BYPASS TAPER.
5. WHEN DISTANCE "C" IS LESS THAN 4 FEET, OMIT DOTTED EXTENSION.
6. WHEN DISTANCE "D" IS LESS THAN 50 FEET, OMIT DOTTED EXTENSION.

LEGEND

DIRECTION OF TRAVEL



INTERSECTION ON OUTSIDE OF CURVE



MAJOR INTERSECTIONS

(INTERSECTION WITH FULL RIGHT TURN LANE OR BYPASS LANE)

PAVEMENT MARKING (INTERSECTIONS)

LEGEND

SIGN ON PERMANENT SUPPORT

TRAFFIC CONTROL DRUM

DIRECTION OF TRAFFIC

WORK ZONE

GENERAL NOTES

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" SIGNS MAY BE USED IF APPROVED BY THE REGIONAL TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

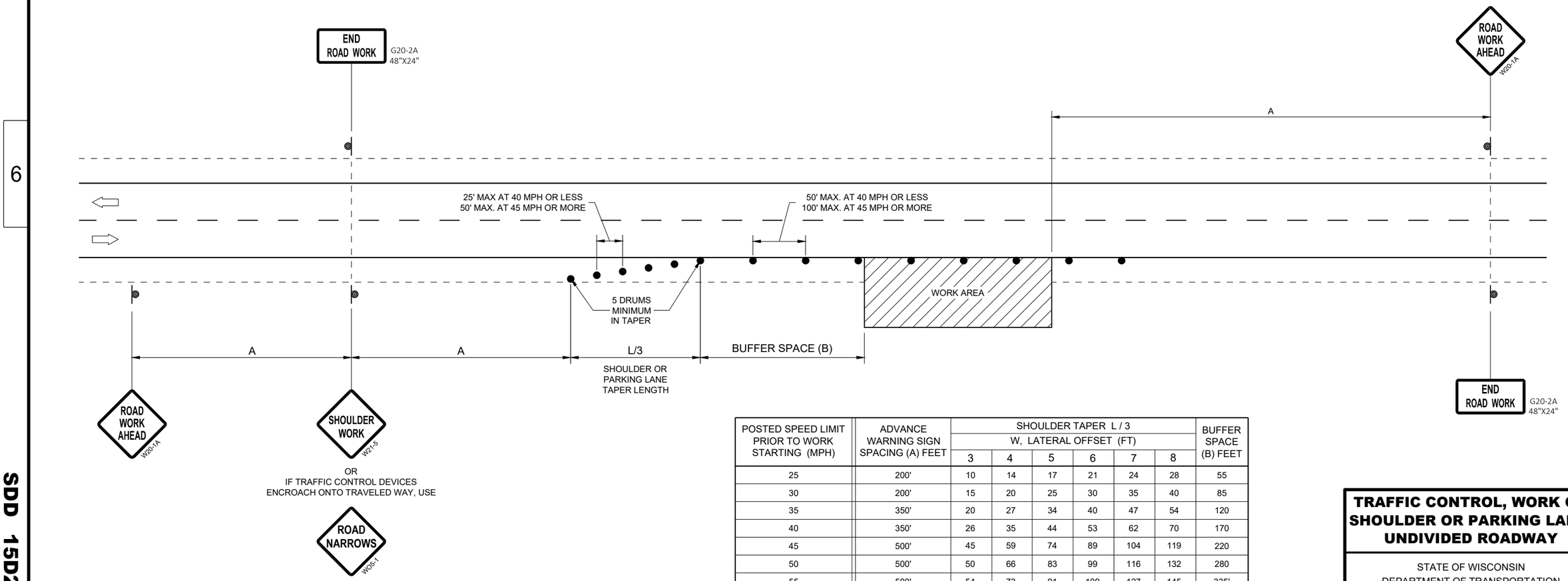
ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

W20-1A AND G20-2A SIGNS ARE NOT REQUIRED IF THE WORK AREA IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT. G20-2A SIGNS MAY ALSO BE OMITTED IF DURATION OF WORK IS LESS THAN 7 CONTINUOUS DAYS AND NIGHTS.



POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET	SHOULDER TAPER L / 3 W, LATERAL OFFSET (FT)						BUFFER SPACE (B) FEET
		3	4	5	6	7	8	
25	200'	10	14	17	21	24	28	55
30	200'	15	20	25	30	35	40	85
35	350'	20	27	34	40	47	54	120
40	350'	26	35	44	53	62	70	170
45	500'	45	59	74	89	104	119	220
50	500'	50	66	83	99	116	132	280
55	500'	54	73	91	109	127	145	335'

TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2020
DATE

/S/ Andrew Heidtke
STATEWIDE WORK ZONE T SAFETY ENGINEER

FHWA

6

SDD 15D28 - 04

GENERAL NOTES

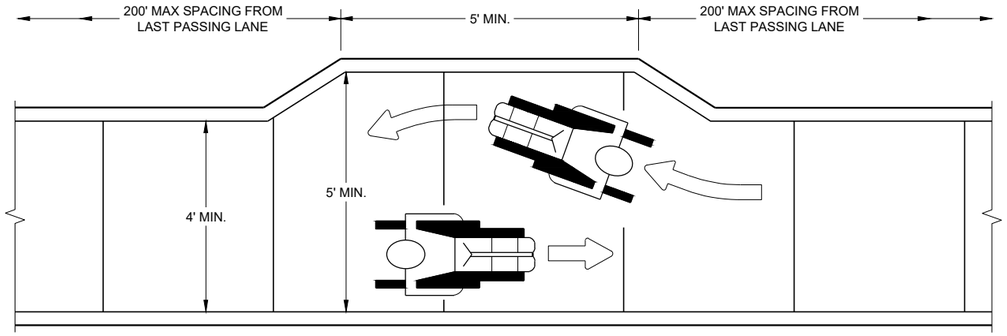
BARRICADE DEVICE SELECTED FROM APPROVED PRODUCT LIST

(1) REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.

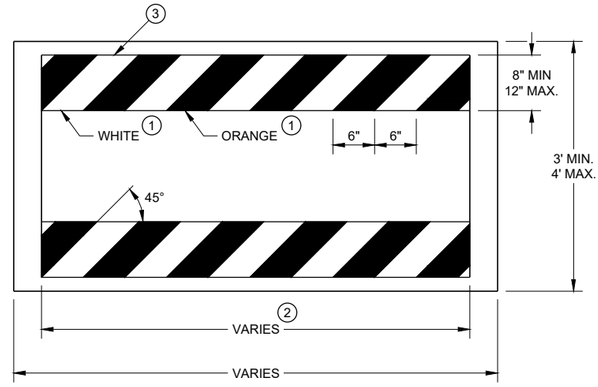
(2) SHEETING REQUIRED ON MORE THAN 50% OF BARRICADE WIDTH.

(3) PLACE SHEETING ON BOTH SIDES OF THE BARRICADE.

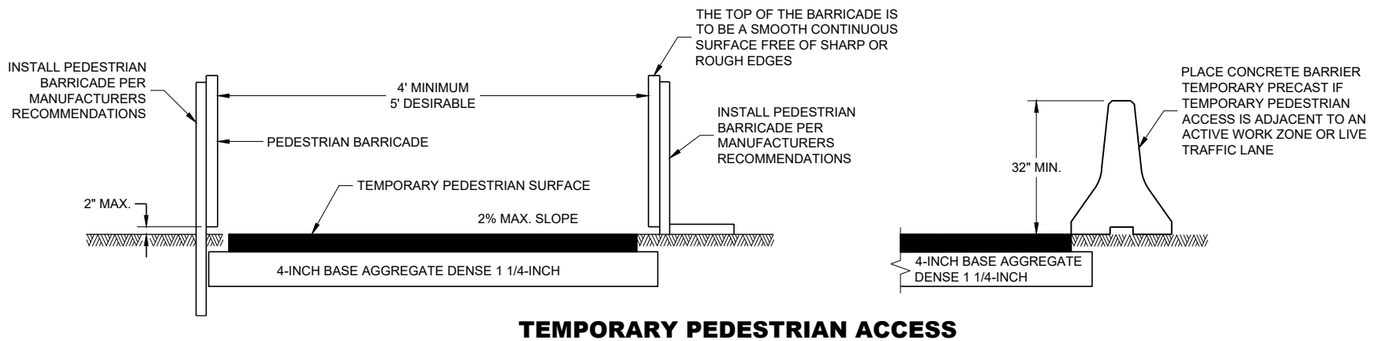
* USE THIS DETAIL FOR SHEETING PLACEMENT REFERENCE.



NARROW SIDEWALK PASSING DETAIL



TEMPORARY PEDESTRIAN BARRICADE*

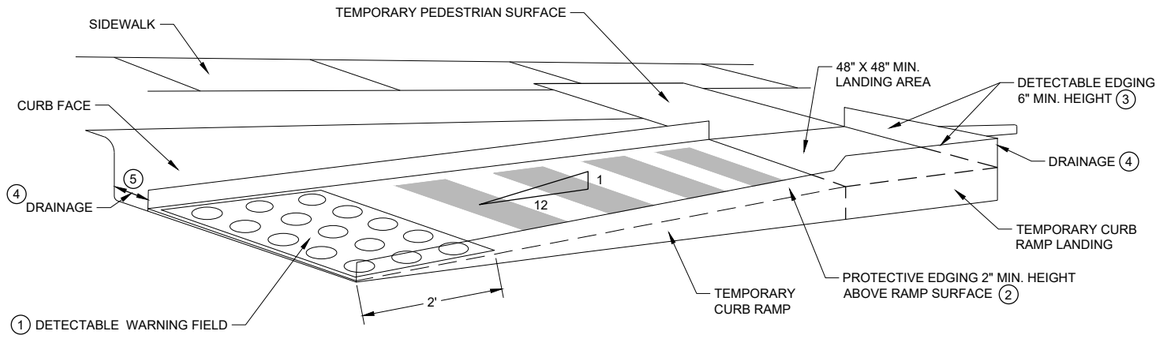


TEMPORARY PEDESTRIAN ACCESS

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION

6

SDD 15D30 - 07a



PERSPECTIVE VIEW

GENERAL NOTES

CURB RAMPS SHALL BE 48" MIN. WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE.

CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.

CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.
LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN ½" WIDTH.

CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED ½". LATERAL EDGES MAY BE VERTICAL UP TO ¼" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN ¼" AND ½".

(1) INSTALL CONTRASTING TEMPORARY DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS, AS SHOWN IN THE PLANS.

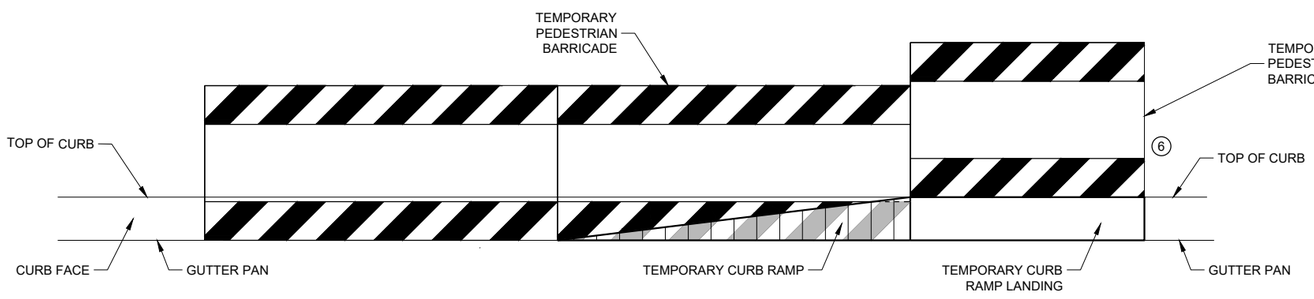
(2) PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.

(3) DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).

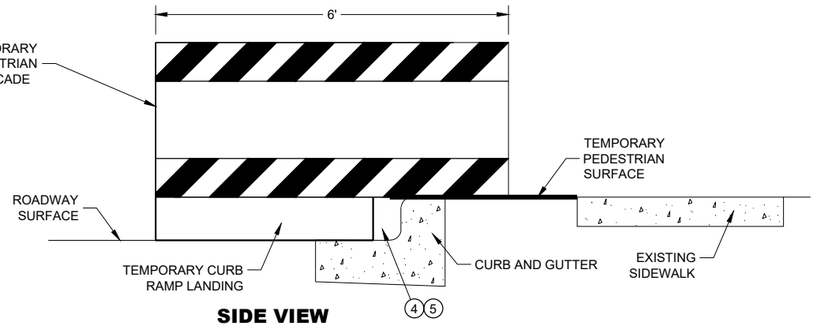
(4) DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.

(5) 6" MINIMUM BETWEEN CURB FACE AND EDGE OF RAMP

(6) IF ONLY PART OF THE END PANEL OF TEMPORARY PEDESTRIAN BARRICADE PANEL IS NEEDED, EXTEND EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL HERE.



FRONT VIEW



SIDE VIEW

TEMPORARY CURB RAMP PARALLEL TO CURB

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

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SDD 15D30 - 07b

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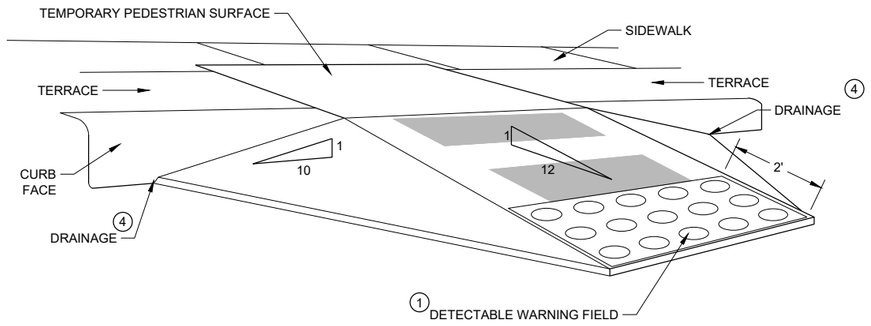
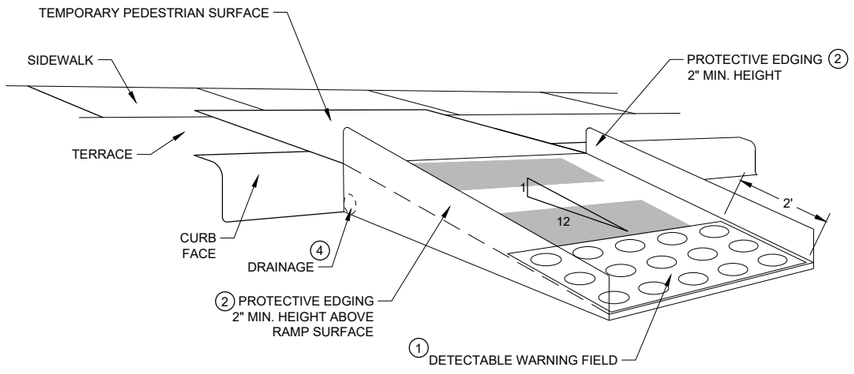


WITH SIDE APRON (5)



WITH PROTECTIVE EDGE

TEMPORARY CURB RAMP PERPENDICULAR TO CURB

GENERAL NOTES

CURB RAMPS SHALL BE 48" MINIMUM WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE.

ALTERNATE SIDEWALK WORK BETWEEN LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.

CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.

CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.

LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN ½" WIDTH.

CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED ½". LATERAL EDGES MAY BE VERTICAL UP TO ¼" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN ¼" AND ½".

(1) INSTALL CONTRASTING TEMPORARY DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS, AS SHOWN IN THE PLANS

(2) PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.

(3) DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).

(4) DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.

(5) CAN ONLY BE USED FOR RAMPS WITH 6" OR LESS OF VERTICAL CHANGE.

GENERAL NOTES

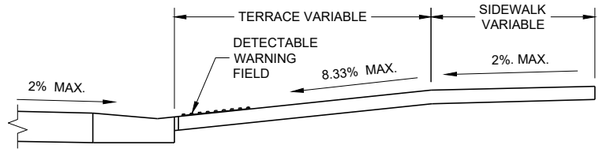
BARRICADE DEVICE SELECTED FROM APPROVED PRODUCT LIST

(1) REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.

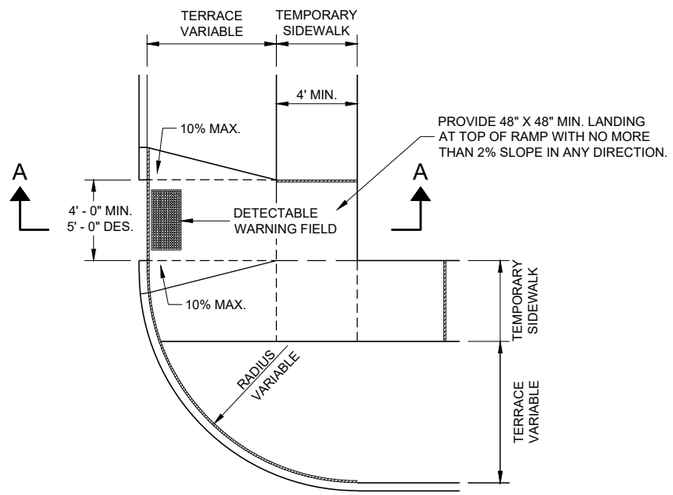
(2) SHEETING REQUIRED ON MORE THAN 50% OF BARRICADE WIDTH.

(3) PLACE SHEETING ON BOTH SIDES OF THE BARRICADE.

* USE THIS DETAIL FOR SHEETING PLACEMENT REFERENCE.



SECTION A - A



PLAN VIEW

TEMPORARY TYPE 3 RAMP

(OUTSIDE OF CROSSWALK AREA)

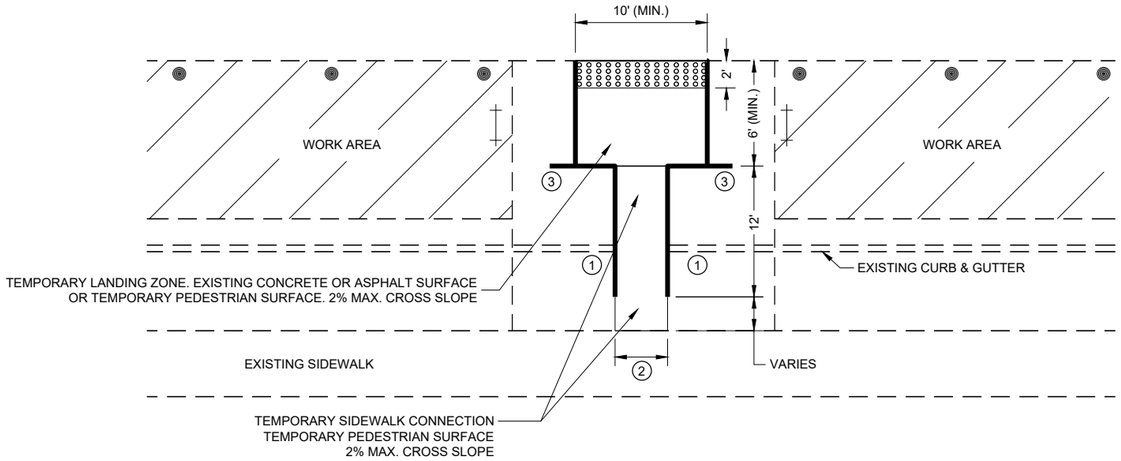
TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

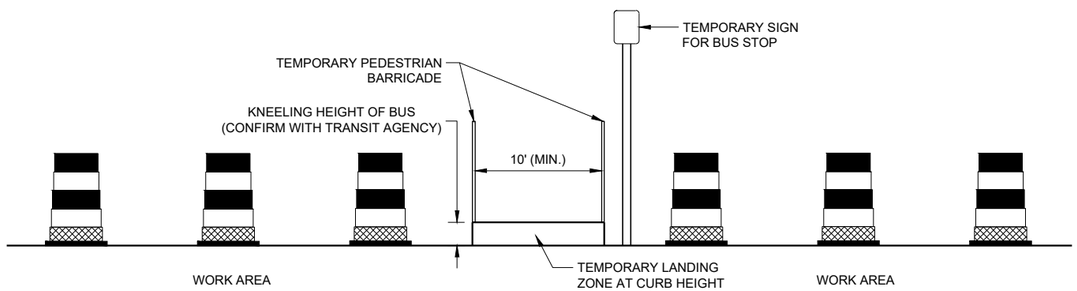
APPROVED
May 2022 — DATE
/S/ Andrew Heidtke — WORK ZONE ENGINEER
FHWA

6

SDD 15D30 - 07d



PLAN VIEW



PROFILE VIEW

TEMPORARY BUS STOP PAD

GENERAL NOTES

TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.

NOTIFY THE BUS COMPANY 7 DAYS IN ADVANCE OF THE BUS STOP RELOCATION.

PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.

DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).

LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN ½" WIDTH.

CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED ½". LATERAL EDGES MAY BE VERTICAL UP TO ¼" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN ¼" AND ½".

CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.

(1) DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.

(2) 5' WIDE MIN. WITH TEMPORARY PEDESTRIAN BARRICADE, 10' WIDE MIN. WITHOUT TEMPORARY PEDESTRIAN BARRICADE.

(3) PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE INTO THIS SPACE.

LEGEND

- TRAFFIC CONTROL DRUM
- TYPE III BARRICADE
- TEMPORARY PEDESTRIAN BARRICADE
- TEMPORARY DETECTABLE WARNING FIELD
- WORK AREA

6

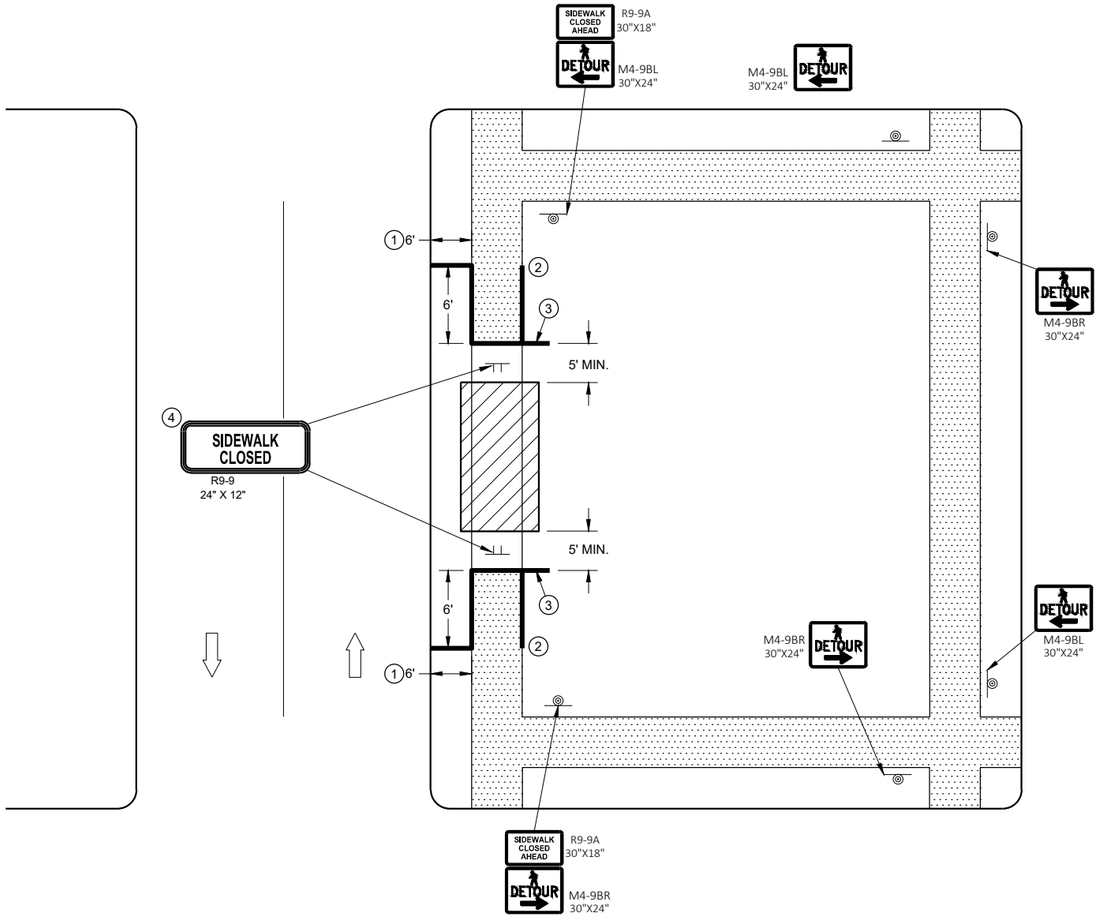
SDD 15D30 - 07e

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTA

LEGEND

- SIGN ON PERMANENT SUPPORT
- SIGN ON TEMPORARY SUPPORT
- UNDER PEDESTRIAN TRAFFIC
- WORK AREA
- TEMPORARY PEDESTRIAN BARRICADE
- DIRECTION OF TRAFFIC



SIDEWALK DETOUR, SIDEWALK ONLY ON ONE SIDE

GENERAL NOTES

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

1. IF TERRACE IS LESS THAN 6 FEET WIDE, OMIT TEMPORARY PEDESTRIAN BARRICADE FROM THE SIDEWALK TO THE CURB.
2. PLACE BARRICADE CLOSURE SO THAT THE TEMPORARY PEDESTRIAN BARRICADE END IS AT THE LAST OPEN SIDEWALK ACCESS TO RESIDENCES OR BUSINESSES BEFORE THE SIDEWALK CLOSURE.
3. IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
4. MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION

6

SDD 15D30 - 07f

LEGEND

- SIGN ON TEMPORARY SUPPORT
- WORK AREA
- UNDER PEDESTRIAN TRAFFIC
- TEMPORARY PEDESTRIAN SURFACE
- TEMPORARY PEDESTRIAN BARRICADE
- OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
- DIRECTION OF TRAFFIC

GENERAL NOTES

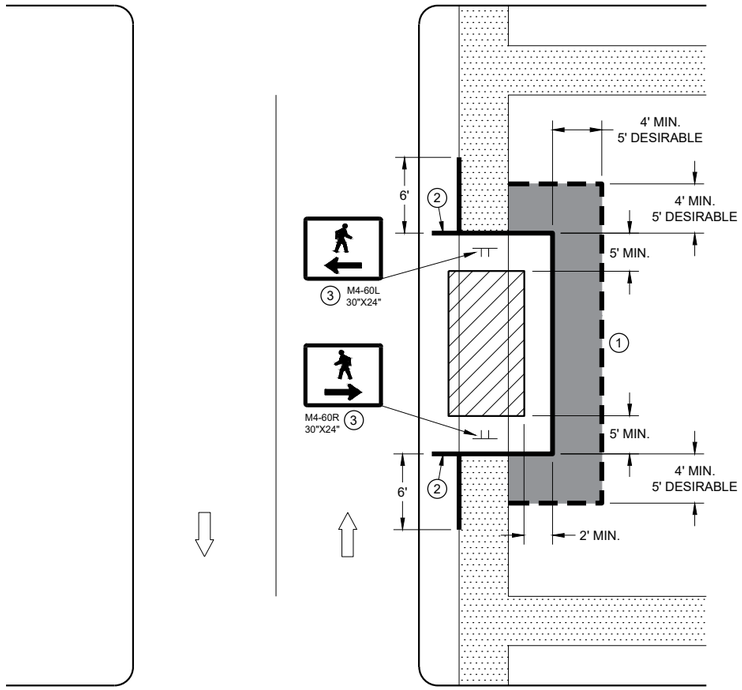
TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

1. USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
2. IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
3. MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.



SIDEWALK DIVERSION
SINGLE SIDE

6

SDD 15D30 - 07g

TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

SDD15D30 - 07g

LEGEND

- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL DRUM
- WORK AREA
- UNDER PEDESTRIAN TRAFFIC
- TEMPORARY CURB RAMP
- TEMPORARY PEDESTRIAN SURFACE "A"
- TEMPORARY PEDESTRIAN SURFACE "B"
- TEMPORARY PEDESTRIAN BARRICADE
- DIRECTION OF TRAFFIC

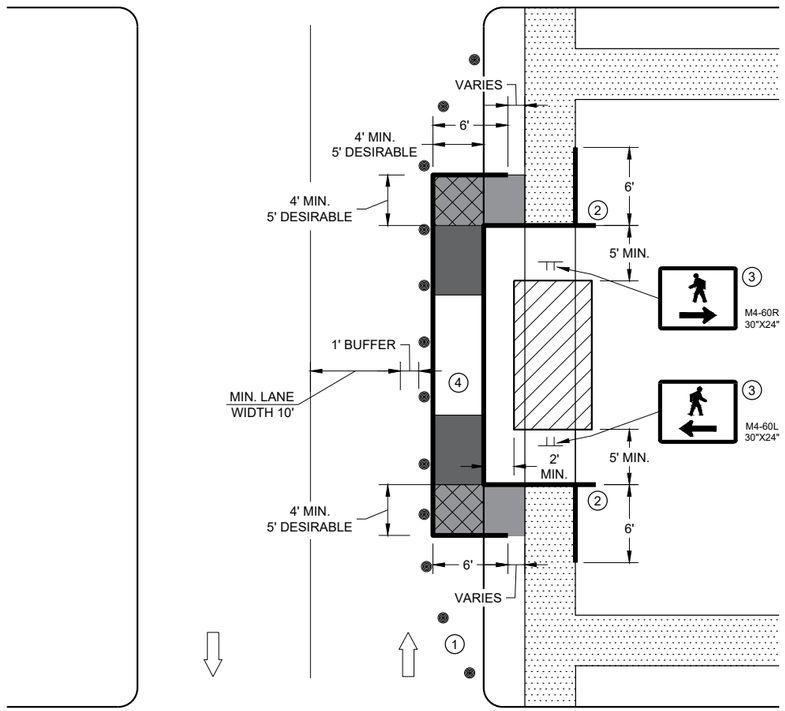
GENERAL NOTES

TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

1. SHOULDER OR LANE CLOSURE ADVANCE WARNING AND BUFFER SPACE REQUIRED.
2. PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL PAST THE SIDEWALK ON THE SIDE AWAY FROM THE ROAD.
3. MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.
4. USE EXISTING PAVEMENT SURFACE. IF EXISTING PAVEMENT SURACE HAS BEEN REMOVED, USE A TEMPORARY PEDESTRIAN SURFACE.



SIDEWALK DIVERSION, SINGLE SIDE

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

SDD 15D30 - 07h

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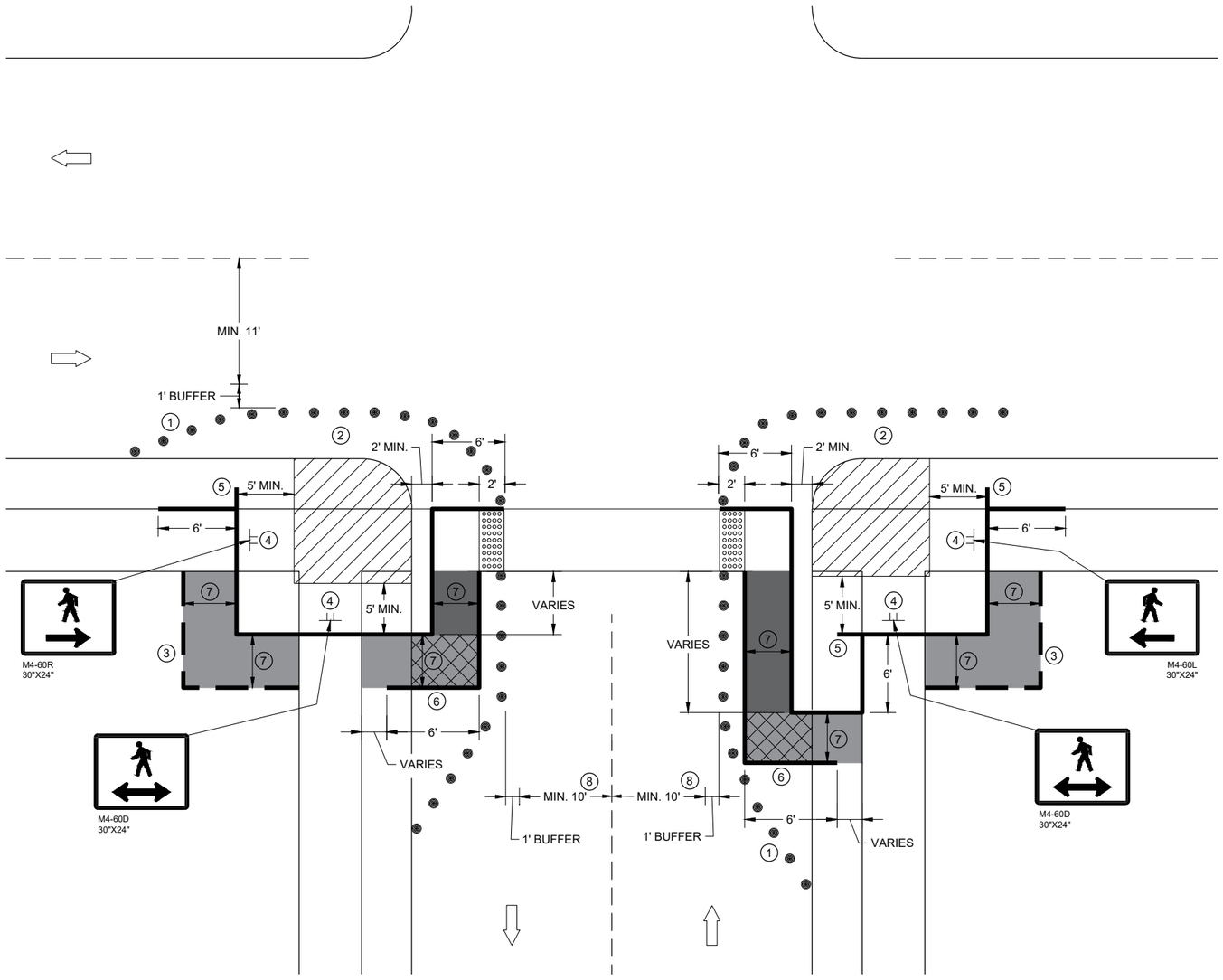


CURB RAMP PEDESTRIAN TRAFFIC CONTROL
SIDEWALK ON SINGLE SIDE

GENERAL NOTES

IF PEDESTRIAN PUSH BUTTONS ARE PRESENT ON THE EXISTING FACILITY, ENSURE THEY ARE MAINTAINED/ACCESSIBLE FOR PEDESTRIAN USE THROUGHOUT THE TEMPORARY PEDESTRIAN ACCOMMODATIONS.

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG

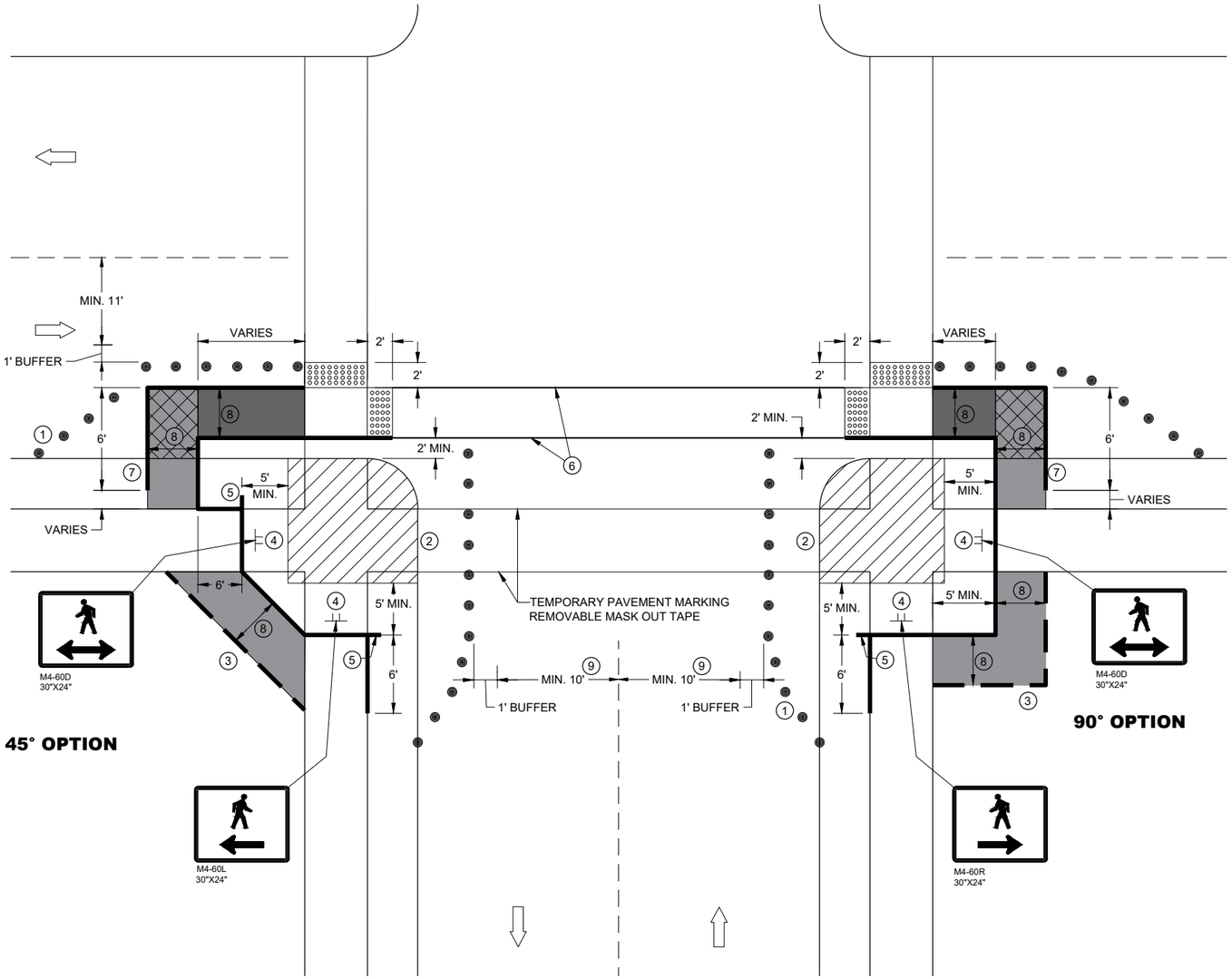
WHEN TEMPORARY PEDESTRIAN BARRICADE RUNS PARALLEL ALONG THE SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

1. SHOULDER OR LANE CLOSURE ADVANCE WARNING AND PROPER BUFFER SPACE REQUIRED.
2. PROVIDE ADEQUATE SPACE FOR CONTRACTOR OPERATIONS
3. USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
4. MOUNTING HEIGHT OF 5 FEET FROM SIDEWALK SURFACE TO BOTTOM OF SIGN.
5. PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL IN THE SIDEWALK TERRACE.
6. IF TEMPORARY PEDESTRIAN BARRICADE DOES NOT REACH THE FACE OF THE CURB, USE AN ADDITIONAL PANEL AND EXTEND INTO THE TERRACE.
7. 4 FEET MINIMUM, 5 FEET DESIRABLE
8. IF MINIMUM LANE WIDTHS CAN'T BE ATTAINED, CURB RAMPS MAY NEED TO BE CONSTRUCTED AT SEPARATE TIMES.

LEGEND

- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL DRUM
- WORK AREA
- TEMPORARY CURB RAMP
- TEMPORARY PEDESTRIAN SURFACE "A"
- TEMPORARY PEDESTRIAN SURFACE "B"
- TEMPORARY DETECTABLE WARNING FIELD
- TEMPORARY PEDESTRIAN BARRICADE
- OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
- DIRECTION OF TRAFFIC

TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION



CURB RAMP PEDESTRIAN TRAFFIC CONTROL

GENERAL NOTES

IF PEDESTRIAN PUSH BUTTONS ARE PRESENT ON THE EXISTING FACILITY, ENSURE THEY ARE MAINTAINED/ACCESSIBLE FOR PEDESTRIAN USE THROUGHOUT THE TEMPORARY PEDESTRIAN ACCOMMODATIONS.

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG

WHEN TEMPORARY PEDESTRIAN BARRICADE RUNS PARALLEL ALONG THE SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

1. SHOULDER OR LANE CLOSURE ADVANCE WARNING AND PROPER BUFFER SPACE REQUIRED.
2. PROVIDE ADEQUATE SPACE FOR CONTRACTOR OPERATIONS
3. USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
4. MOUNTING HEIGHT OF 5 FEET FROM SIDEWALK SURFACE TO BOTTOM OF SIGN.
5. PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL IN THE SIDEWALK TERRACE.
6. WHITE 6" TEMPORARY PAVEMENT MARKING
7. IF TEMPORARY PEDESTRIAN BARRICADE DOES NOT REACH THE FACE OF THE CURB, USE AN ADDITIONAL PANEL AND EXTEND INTO THE TERRACE.
8. 4 FEET MINIMUM, 5 FEET DESIRABLE
9. IF MINIMUM LANE WIDTHS CAN'T BE ATTAINED, CURB RAMPS MAY NEED TO BE CONSTRUCTED AT SEPARATE TIMES.

LEGEND

- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL DRUM
- WORK AREA
- TEMPORARY CURB RAMP
- TEMPORARY PEDESTRIAN SURFACE "A"
- TEMPORARY PEDESTRIAN SURFACE "B"
- TEMPORARY DETECTABLE WARNING FIELD
- TEMPORARY PEDESTRIAN BARRICADE
- OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
- DIRECTION OF TRAFFIC

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION

6
SDD 15D30 - 07j

DIVISION 1 - STH 80 WAYSIDE STATION	REAL STATION	DISTANCE	AREA (SF)				INCREMENTAL VOL (CY) (UNADJUSTED)				CUMULATIVE VOL (CY)				
			CUT	SALVAGED/UNUSABLE PAVEMENT MATERIAL	FILL	EBS	CUT	SALVAGED/UNUSABLE PAVEMENT MATERIAL	FILL	EBS	CUT	EXPANDED FILL	EXPANDED EBS BACKFILL	REDUCED EBS IN FILL	MASS ORDINATE
							NOTE 1	NOTE 2	NOTE 3		1.00 NOTE 1	1.25	1.30 NOTE 5	0.80 NOTE 7	NOTE 8
648+50	64850.00	0.00	11.44	10.00	8.39	0.00	0	0	0	0	0	0	0	0	0
649+00	64900.00	50.00	31.23	10.00	8.22	0.00	40	19	15	0	40	19	0	0	2
649+50	64950.00	50.00	18.83	10.00	6.06	0.00	46	19	13	0	86	35	0	0	13
650+00	65000.00	50.00	18.38	10.00	9.63	0.00	34	19	15	0	120	54	0	0	9
650+50	65050.00	50.00	19.65	10.00	3.93	0.00	35	19	13	0	155	70	0	0	9
651+00	65100.00	50.00	37.30	10.00	0.02	0.00	53	19	4	0	208	75	0	0	38
651+50	65150.00	50.00	58.96	10.00	0.00	0.00	89	19	0	0	297	75	0	0	108
652+00	65200.00	50.00	84.41	10.00	0.00	0.00	133	19	0	0	430	75	0	0	222
652+50	65250.00	50.00	92.49	10.00	0.00	0.00	164	19	0	0	594	75	0	0	367
653+00	65300.00	50.00	41.86	10.00	0.01	0.00	124	19	0	0	718	75	0	0	472
653+50	65350.00	50.00	33.31	10.00	0.23	0.00	70	19	0	0	788	75	0	0	523
654+00	65400.00	50.00	33.98	10.00	0.14	0.00	62	19	0	0	850	75	0	0	566
654+50	65450.00	50.00	38.56	10.00	0.11	0.00	67	19	0	0	917	75	0	0	614
655+00	65500.00	50.00	33.25	10.00	0.01	0.00	66	19	0	0	983	75	0	0	661
655+50	65550.00	50.00	33.51	10.00	0.01	0.00	62	19	0	0	1,045	75	0	0	704
656+00	65600.00	50.00	34.30	10.00	0.00	0.00	63	19	0	0	1,108	75	0	0	748
656+50	65650.00	50.00	34.17	10.00	0.00	0.00	63	19	0	0	1,171	75	0	0	792
657+00	65700.00	50.00	34.33	10.00	0.00	0.00	63	19	0	0	1,234	75	0	0	836
657+50	65750.00	50.00	35.31	10.00	0.00	0.00	64	19	0	0	1,298	75	0	0	881
658+00	65800.00	50.00	50.57	10.00	0.00	0.00	80	19	0	0	1,378	75	0	0	942
658+50	65850.00	50.00	51.49	10.00	0.00	0.00	94	19	0	0	1,472	75	0	0	1,017
659+00	65900.00	50.00	53.03	10.00	0.00	0.00	97	19	0	0	1,569	75	0	0	1,095
659+50	65950.00	50.00	57.96	10.00	0.00	0.00	103	19	0	0	1,672	75	0	0	1,179
660+00	66000.00	50.00	50.48	10.00	0.00	0.00	100	19	0	0	1,772	75	0	0	1,260
660+50	66050.00	50.00	42.75	10.00	0.00	0.00	86	19	0	0	1,858	75	0	0	1,327
661+00	66100.00	50.00	34.30	10.00	0.00	0.00	71	19	0	0	1,929	75	0	0	1,379
661+50	66150.00	50.00	28.14	10.00	0.00	0.00	58	19	0	0	1,987	75	0	0	1,418
662+00	66200.00	50.00	27.53	10.00	0.00	0.00	52	19	0	0	2,039	75	0	0	1,451
662+50	66250.00	50.00	22.05	10.00	0.00	0.00	46	19	0	0	2,085	75	0	0	1,478
663+00	66300.00	50.00	24.94	10.00	0.00	0.00	44	19	0	0	2,129	75	0	0	1,503

Notes:

1 - CUT	CUT INCLUDES SALVAGED/UNUSABLE PAVEMENT MATERIAL
2 - SALVAGED/UNUSABLE PAVEMENT MATERIAL	THIS DOES NOT SHOW UP IN CROSS SECTIONS
3 - FILL	DOES NOT INCLUDE UNUSABLE PAVEMENT EXC VOLUME
4 - EXPANDED MARSH BACKFILL	WILL BE BACKFILLED WITH GRANULAR BACKFILL (OR CUT, OR BORROW)
5 - EXPANDED EBS	WILL BE BACKFILLED WITH GRANULAR BACKFILL (OR CUT, OR BORROW)
6 - REDUCED MARSH IN FILL	REDUCED MARSH EXCAVATION THAT CAN BE USED IN FILL
7 - REDUCED EBS IN FILL	REDUCED EBS EXCAVATION THAT CAN BE USED IN FILL
8 - MASS ORDINATE	IF MARSH OR EBS TO BE BACKFILLED WITH COMMON OR BORROW: [(CUT - SALVAGED PAVT - EXPANDED MARSH EXC - EXPANDED EBS) - ((FILL - REDUCED MARSH IN FILL - REDUCED EBS IN FILL - EXPANDED ROCK) * FILL FACTOR)]
8 - MASS ORDINATE	IF MARSH AND EBS TO BE BACKFILLED WITH GRANULAR: [CUT - SALVAGED PAVT - ((FILL - REDUCED MARSH IN FILL - REDUCED EBS IN FILL - EXPANDED ROCK) * FILL FACTOR)]
8 - MASS ORDINATE	IF MARSH AND EBS TO BE BACKFILLED WITH COMMON OR BORROW: [(CUT - SALVAGED PAVT - EXPANDED MARSH EXC - EXPANDED EBS) - ((FILL - EXPANDED ROCK) * FILL FACTOR)]
8 - MASS ORDINATE	IF MARSH AND EBS TO BE BACKFILLED WITH GRANULAR: [CUT - SALVAGED PAVT - ((FILL - EXPANDED ROCK) * FILL FACTOR)]

NOTE 4 - SELECT ONE BASED ON INPUT DIALOG SELECTION
NOTE 5 - SELECT ONE BASED ON INPUT DIALOG SELECTION
NOTE 6 - IF EXCAVATED MARSH CAN BE USED IN FILL
NOTE 7 - IF EXCAVATED EBS CAN BE USED IN FILL
NOTE 8 - SELECT ONE BASED ON MASS HAUL INPUT DIALOG SELECTION. EBS AND MARSH EXC USED OUTSIDE 1:1 IN FILL SLOPES
EBS AND MARSH EXC USED OUTSIDE 1:1 IN FILL SLOPES
MARSH AND EBS ARE NOT USABLE OUTSIDE THE 1:1 SLOPES
MARSH AND EBS ARE NOT USABLE OUTSIDE THE 1:1 SLOPES

DIVISION 1 - STH 80 PIPE STATION	REAL STATION	DISTANCE	AREA (SF)				INCREMENTAL VOL (CY) (UNADJUSTED)				CUMULATIVE VOL (CY)				
			CUT	SALVAGED/UNUSABLE PAVEMENT MATERIAL	FILL	EBS	CUT	SALVAGED/UNUSABLE PAVEMENT MATERIAL	FILL	EBS	CUT	EXPANDED FILL	EXPANDED EBS BACKFILL	REDUCED EBS IN FILL	MASS ORDINATE
							NOTE 1	NOTE 2	NOTE 3		1.00 NOTE 1	1.25	1.30 NOTE 5	0.80 NOTE 7	NOTE 8
746+94.913	74694.91	0.00	43.97	10.00	26.85	0.00	0	0	0	0	0	0	0	0	0
747+25	74725.00	30.09	48.77	10.00	11.26	0.00	52	11	21	0	52	26	0	0	15
747+59.204	74759.20	34.20	59.92	10.00	0.74	0.00	69	13	8	0	121	36	0	0	61
748+00	74800.00	40.80	45.02	10.00	40.15	0.00	79	15	31	0	200	75	0	0	86
748+23.746	74823.75	23.75	42.32	10.00	50.79	0.00	38	9	40	0	238	125	0	0	65

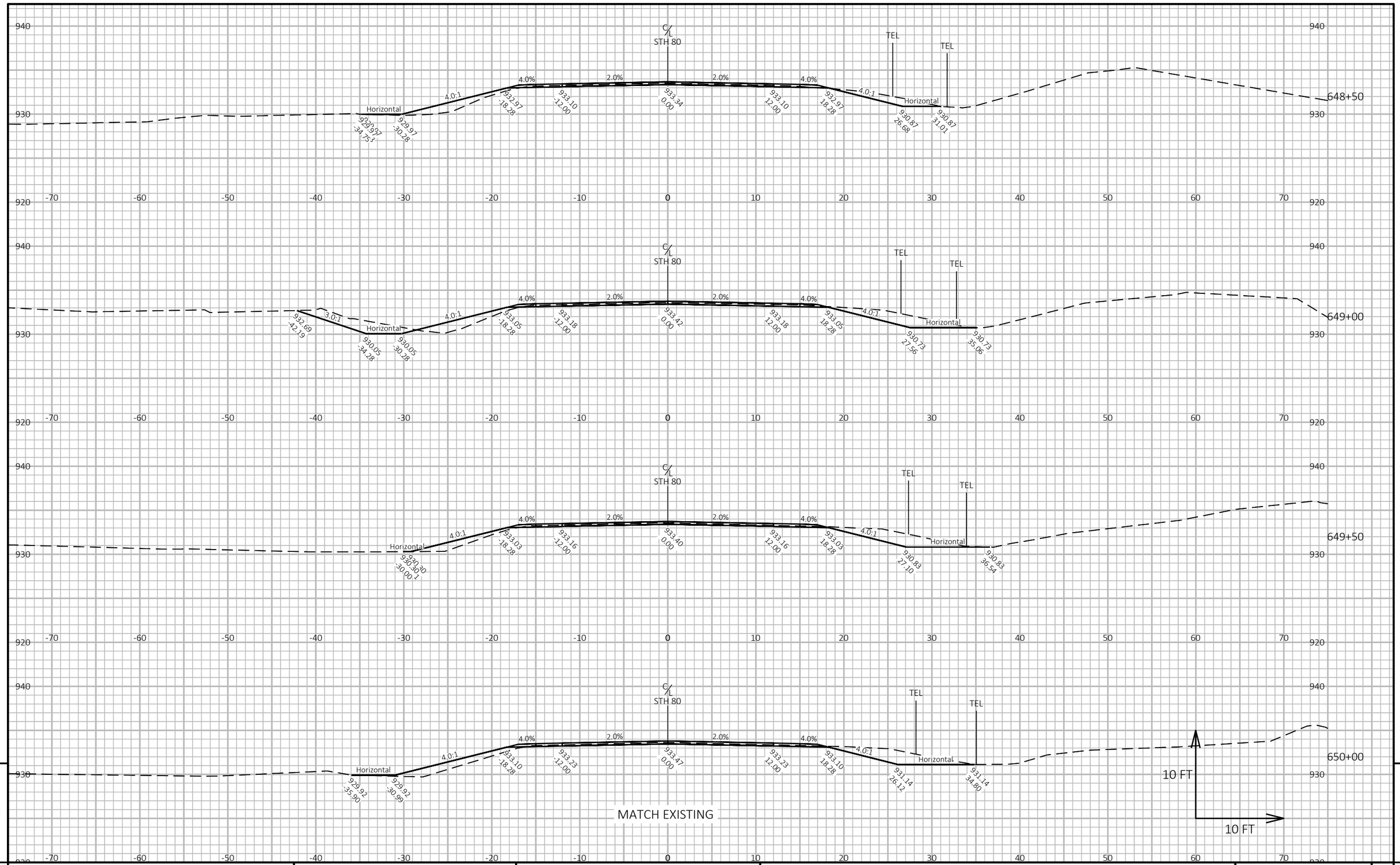
Notes:

1 - CUT	CUT INCLUDES SALVAGED/UNUSABLE PAVEMENT MATERIAL
2 - SALVAGED/UNUSABLE PAVEMENT MATERIAL	THIS DOES NOT SHOW UP IN CROSS SECTIONS
3 - FILL	DOES NOT INCLUDE UNUSABLE PAVEMENT EXC VOLUME
4 - EXPANDED MARSH BACKFILL	WILL BE BACKFILLED WITH GRANULAR BACKFILL (OR CUT, OR BORROW)
5 - EXPANDED EBS	WILL BE BACKFILLED WITH GRANULAR BACKFILL (OR CUT, OR BORROW)
6 - REDUCED MARSH IN FILL	REDUCED MARSH EXCAVATION THAT CAN BE USED IN FILL
7 - REDUCED EBS IN FILL	REDUCED EBS EXCAVATION THAT CAN BE USED IN FILL
8 - MASS ORDINATE	IF MARSH OR EBS TO BE BACKFILLED WITH COMMON OR BORROW: [(CUT - SALVAGED PAVT - EXPANDED MARSH EXC - EXPANDED EBS) - ((FILL - REDUCED MARSH IN FILL - REDUCED EBS IN FILL - EXPANDED ROCK) * FILL FACTOR)]
8 - MASS ORDINATE	IF MARSH AND EBS TO BE BACKFILLED WITH GRANULAR: [CUT - SALVAGED PAVT - ((FILL - REDUCED MARSH IN FILL - REDUCED EBS IN FILL - EXPANDED ROCK) * FILL FACTOR)]
8 - MASS ORDINATE	IF MARSH AND EBS TO BE BACKFILLED WITH COMMON OR BORROW: [(CUT - SALVAGED PAVT - EXPANDED MARSH EXC - EXPANDED EBS) - ((FILL - EXPANDED ROCK) * FILL FACTOR)]
8 - MASS ORDINATE	IF MARSH AND EBS TO BE BACKFILLED WITH GRANULAR: [CUT - SALVAGED PAVT - ((FILL - EXPANDED ROCK) * FILL FACTOR)]

NOTE 4 - SELECT ONE BASED ON INPUT DIALOG SELECTION
NOTE 5 - SELECT ONE BASED ON INPUT DIALOG SELECTION
NOTE 6 - IF EXCAVATED MARSH CAN BE USED IN FILL
NOTE 7 - IF EXCAVATED EBS CAN BE USED IN FILL
NOTE 8 - SELECT ONE BASED ON MASS HAUL INPUT DIALOG SELECTION. EBS AND MARSH EXC USED OUTSIDE 1:1 IN FILL SLOPES
EBS AND MARSH EXC USED OUTSIDE 1:1 IN FILL SLOPES
MARSH AND EBS ARE NOT USABLE OUTSIDE THE 1:1 SLOPES
MARSH AND EBS ARE NOT USABLE OUTSIDE THE 1:1 SLOPES

9

9



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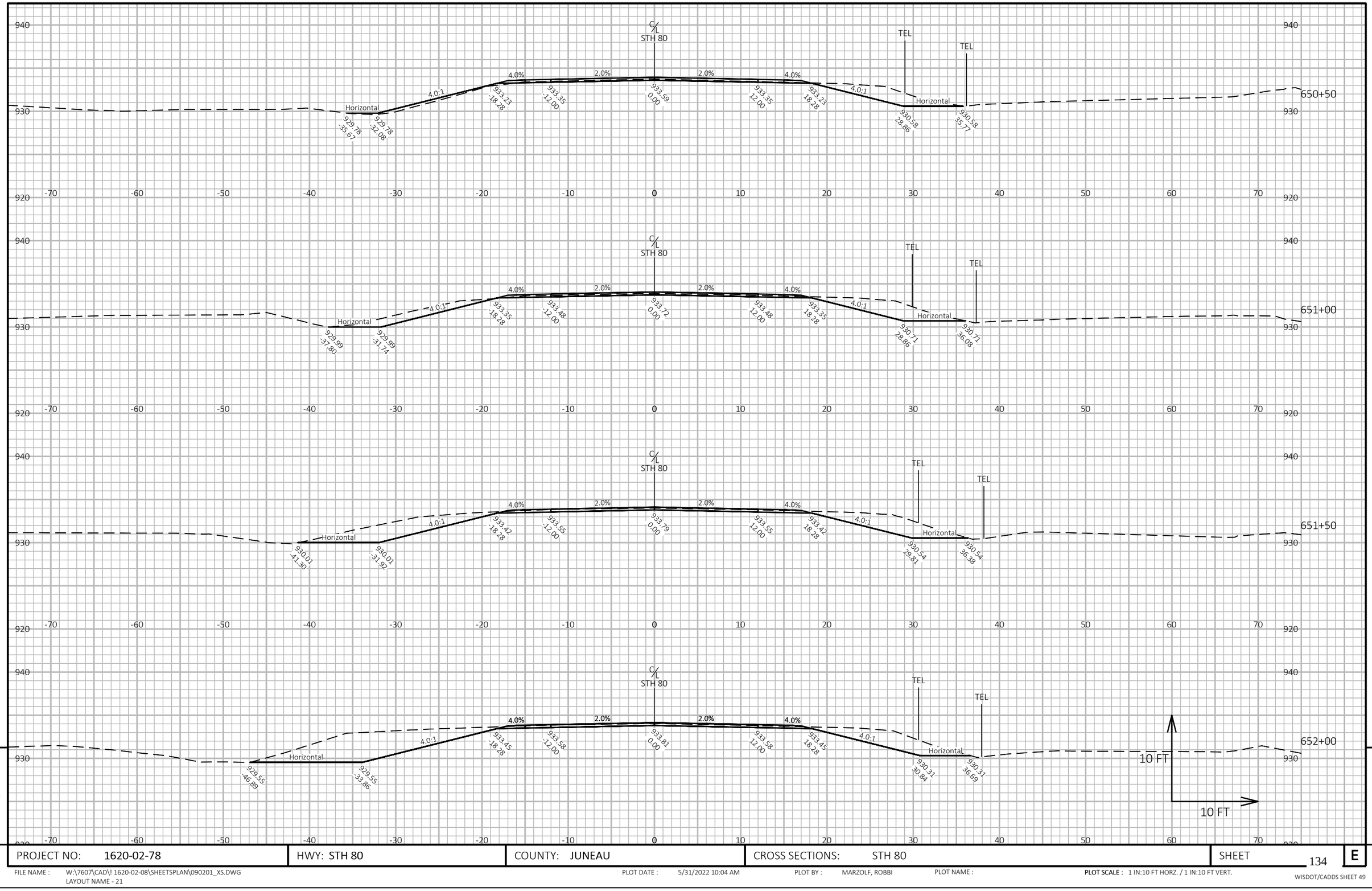
9

PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	CROSS SECTIONS: STH 80	SHEET 133	E

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\090201_XS.DWG
LAYOUT NAME - 20

PLOT DATE : 5/31/2022 10:04 AM PLOT BY : MARZOLF, ROBBI PLOT NAME : PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.



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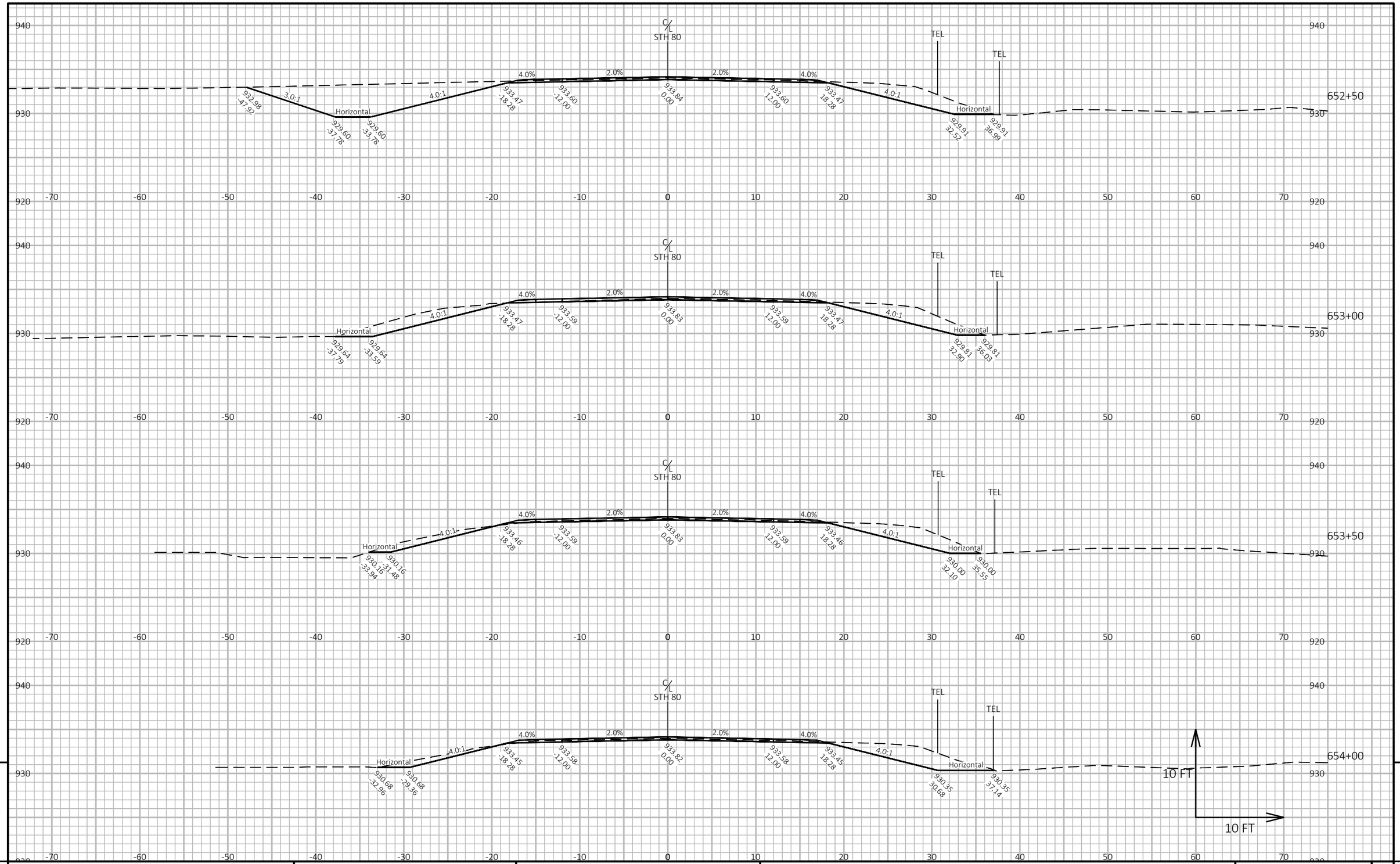
PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	CROSS SECTIONS: STH 80	SHEET 134	E

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\090201_XS.DWG
LAYOUT NAME - 21

PLOT DATE : 5/31/2022 10:04 AM PLOT BY : MARZOLF, ROBBI PLOT NAME : **PLOT SCALE :** 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.



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PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	CROSS SECTIONS: STH 80	SHEET 135	E

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\090201_XS.DWG
LAYOUT NAME - 22

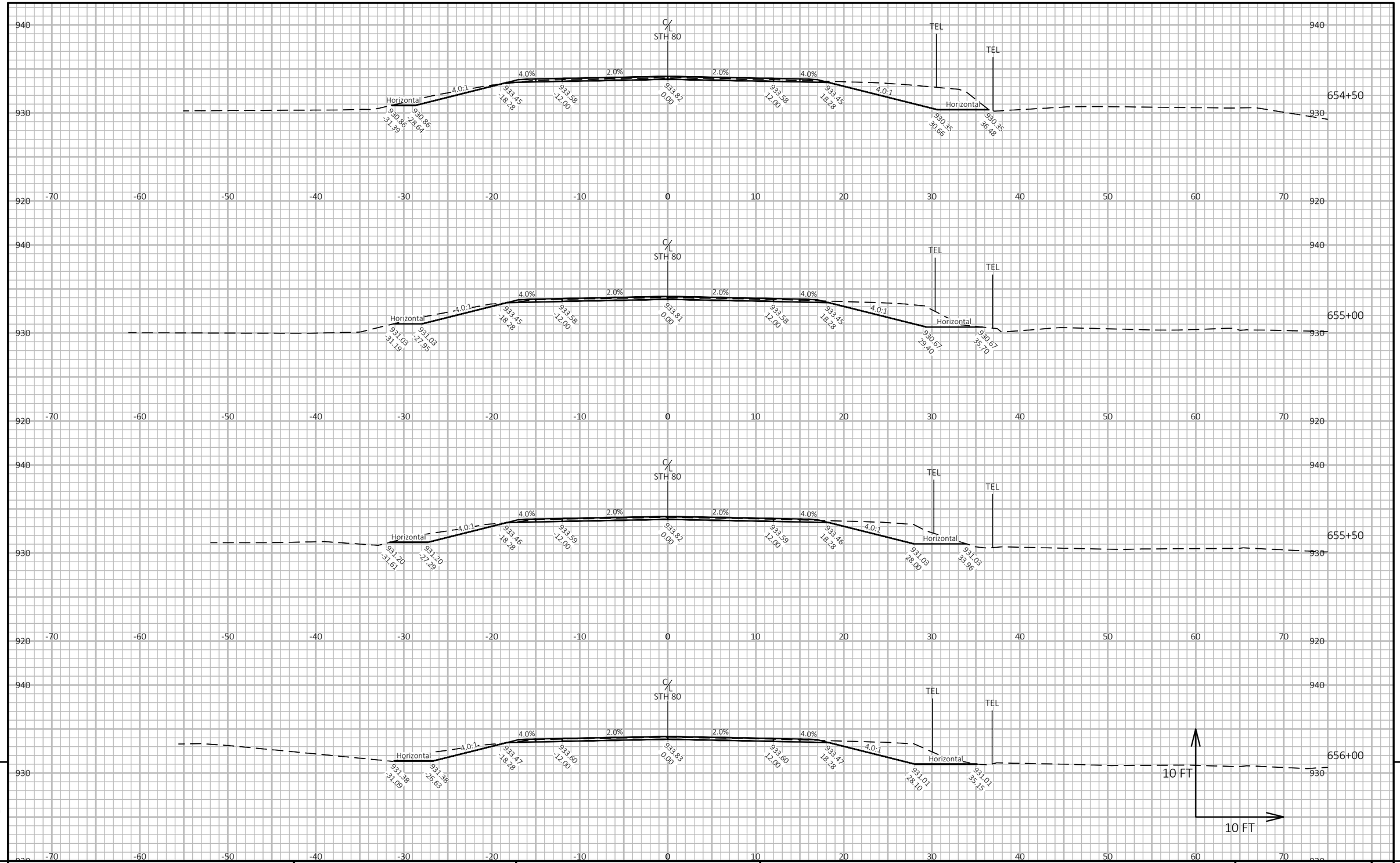
PLOT DATE : 5/31/2022 10:04 AM

PLOT BY : MARZOLF, ROBBI

PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



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PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	CROSS SECTIONS: STH 80	SHEET 136	E

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\090201_XS.DWG
LAYOUT NAME - 23

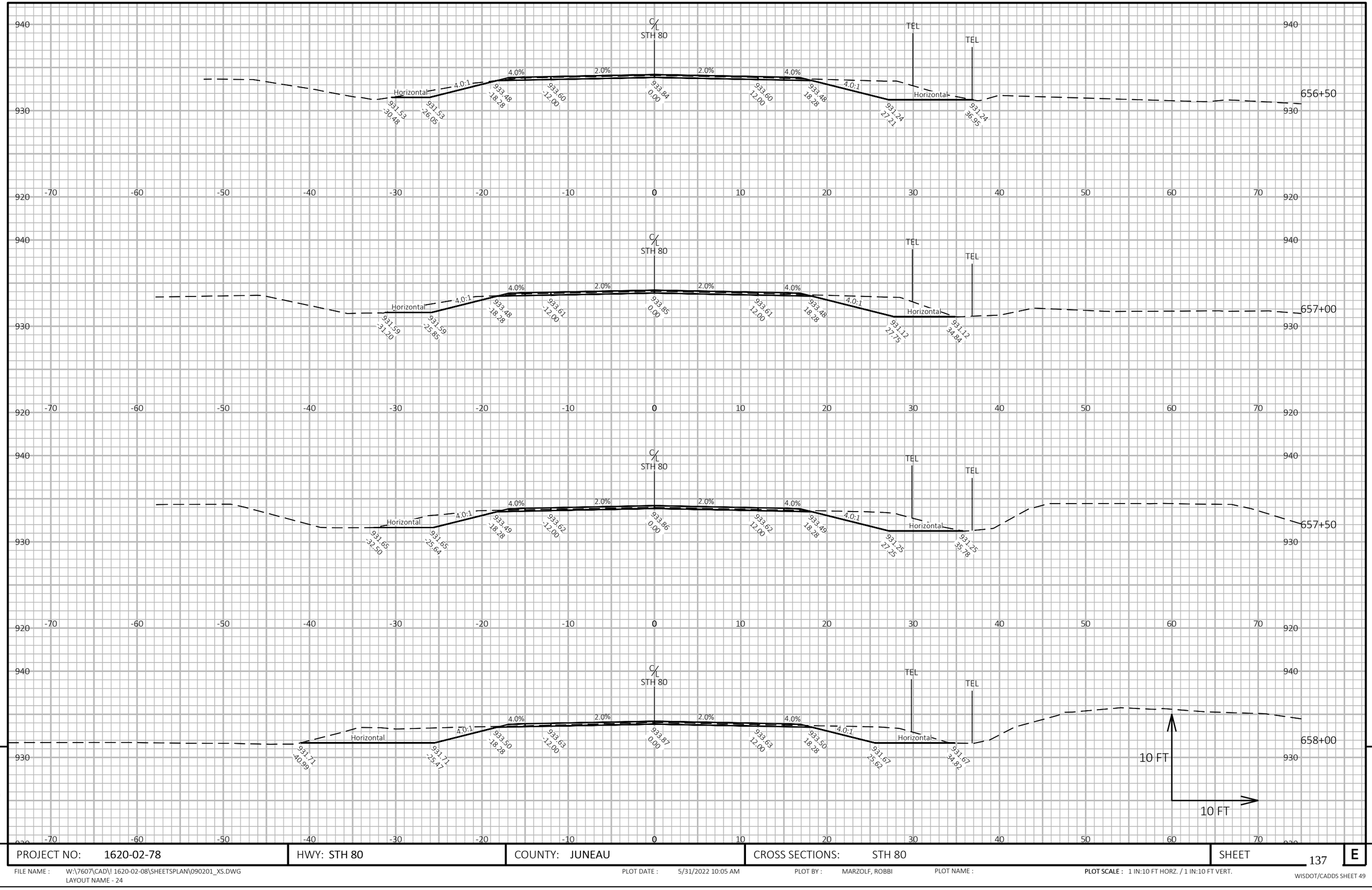
PLOT DATE : 5/31/2022 10:05 AM

PLOT BY : MARZOLF, ROBBI

PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



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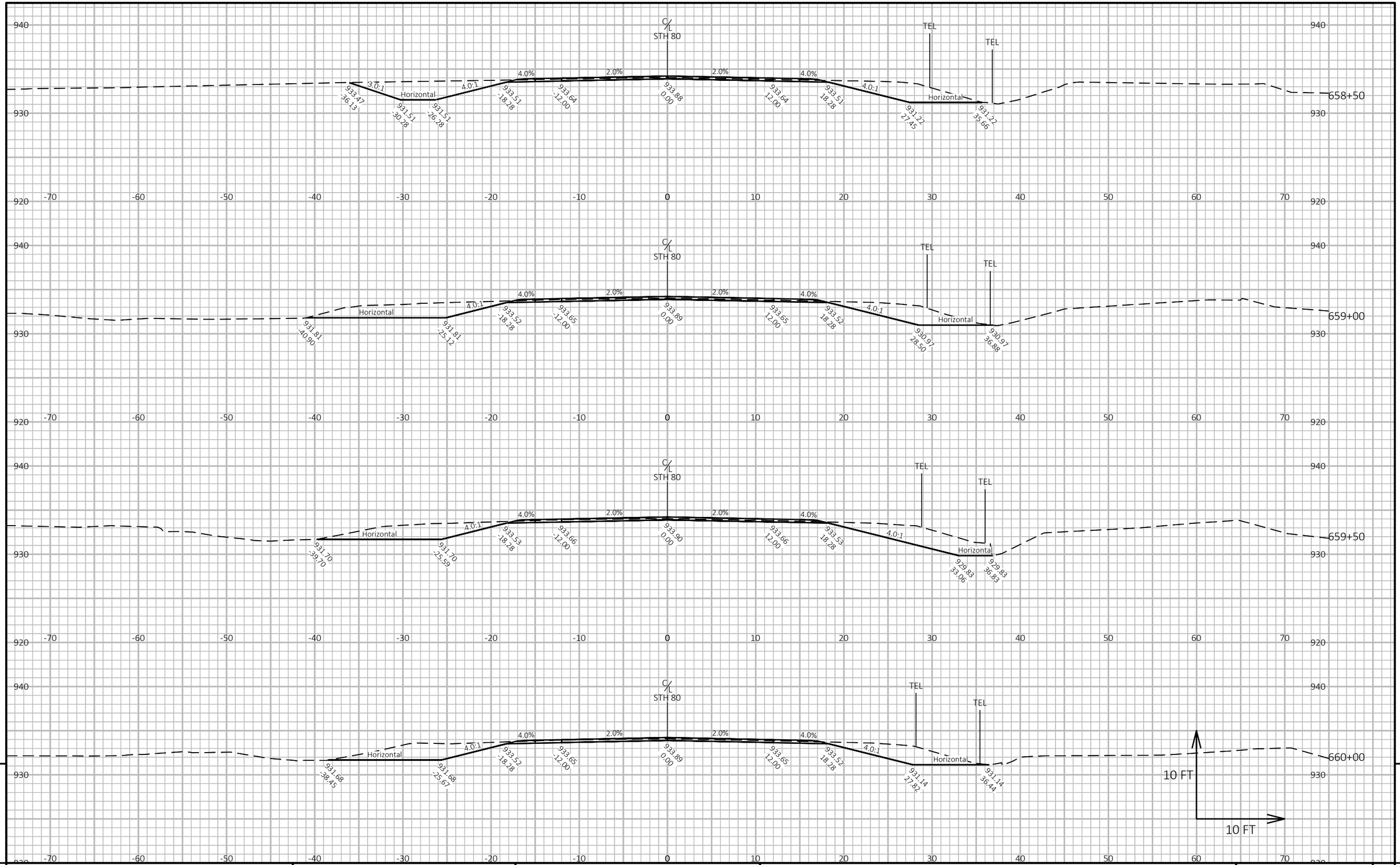
PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	CROSS SECTIONS: STH 80	SHEET 137	E

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\090201_XS.DWG
LAYOUT NAME - 24

PLOT DATE : 5/31/2022 10:05 AM PLOT BY : MARZOLF, ROBBI PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



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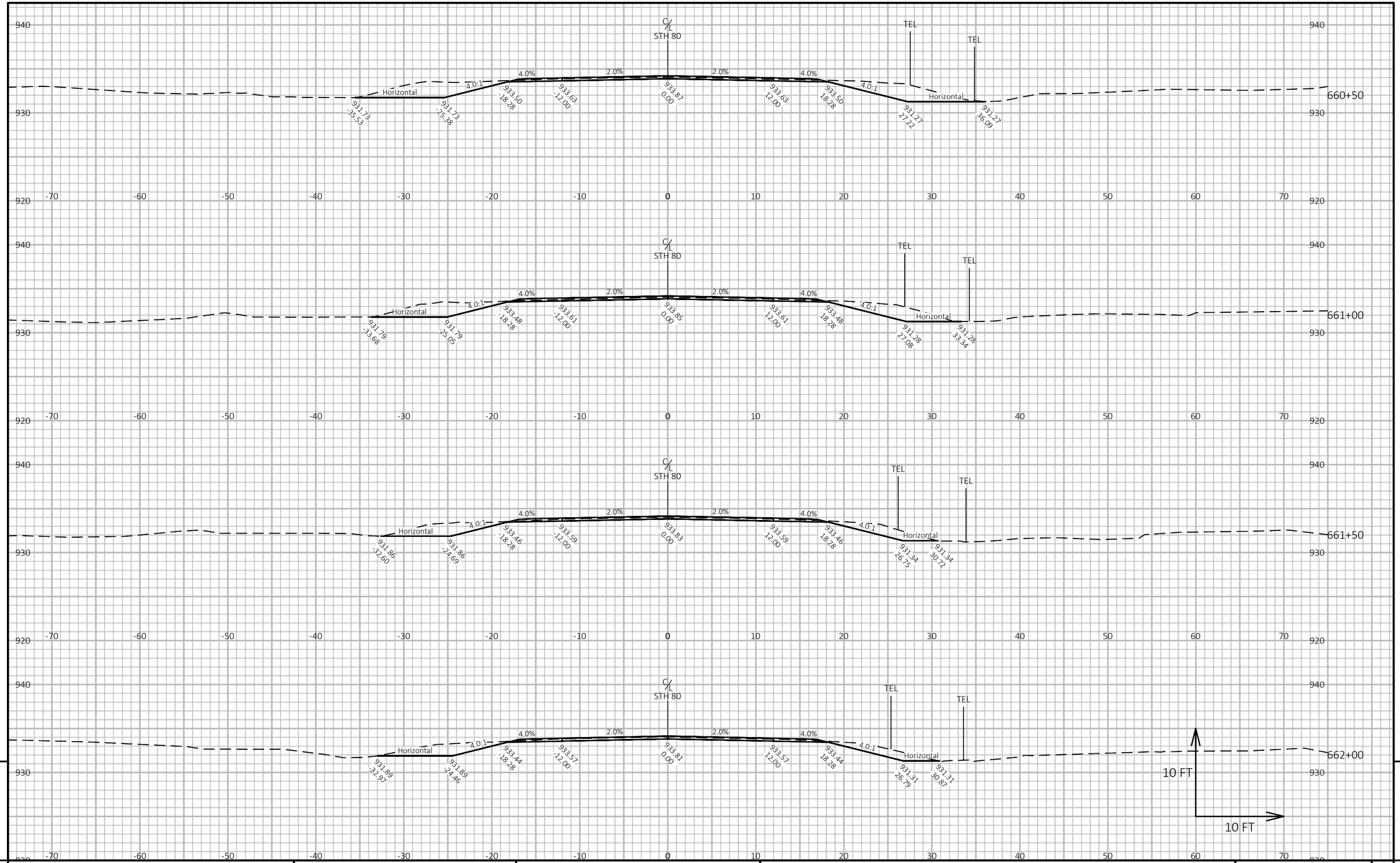
PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	CROSS SECTIONS: STH 80	SHEET 138	E

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\090201_XS.DWG
LAYOUT NAME - 25

PLOT DATE : 5/31/2022 10:05 AM PLOT BY : MARZOLF, ROBBI PLOT NAME : PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.



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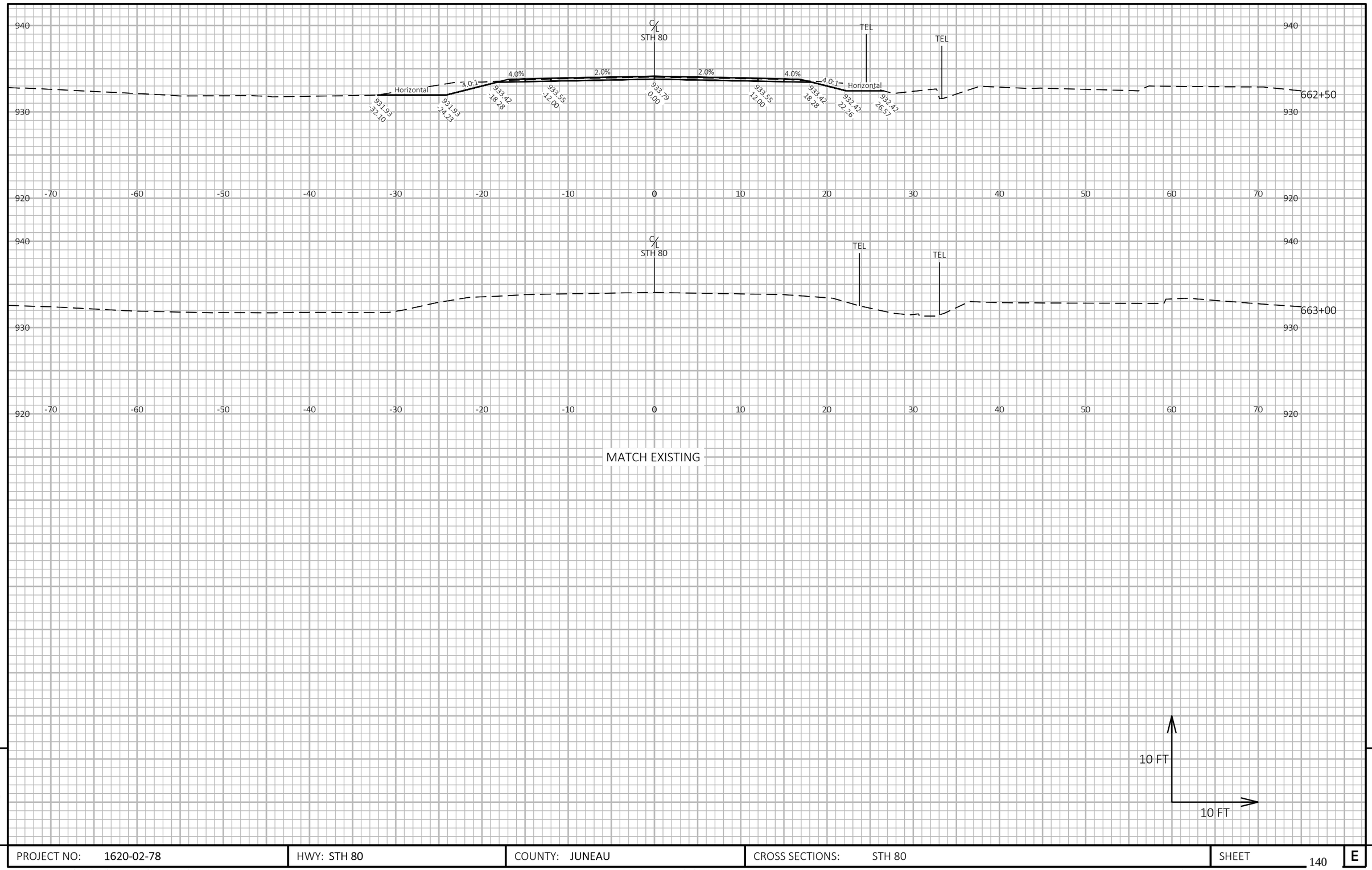
PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	CROSS SECTIONS: STH 80	SHEET 139	E

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\090201_XS.DWG
LAYOUT NAME - 26

PLOT DATE : 5/31/2022 10:05 AM PLOT BY : MARZOLF, ROBBI PLOT NAME : **PLOT SCALE :** 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.



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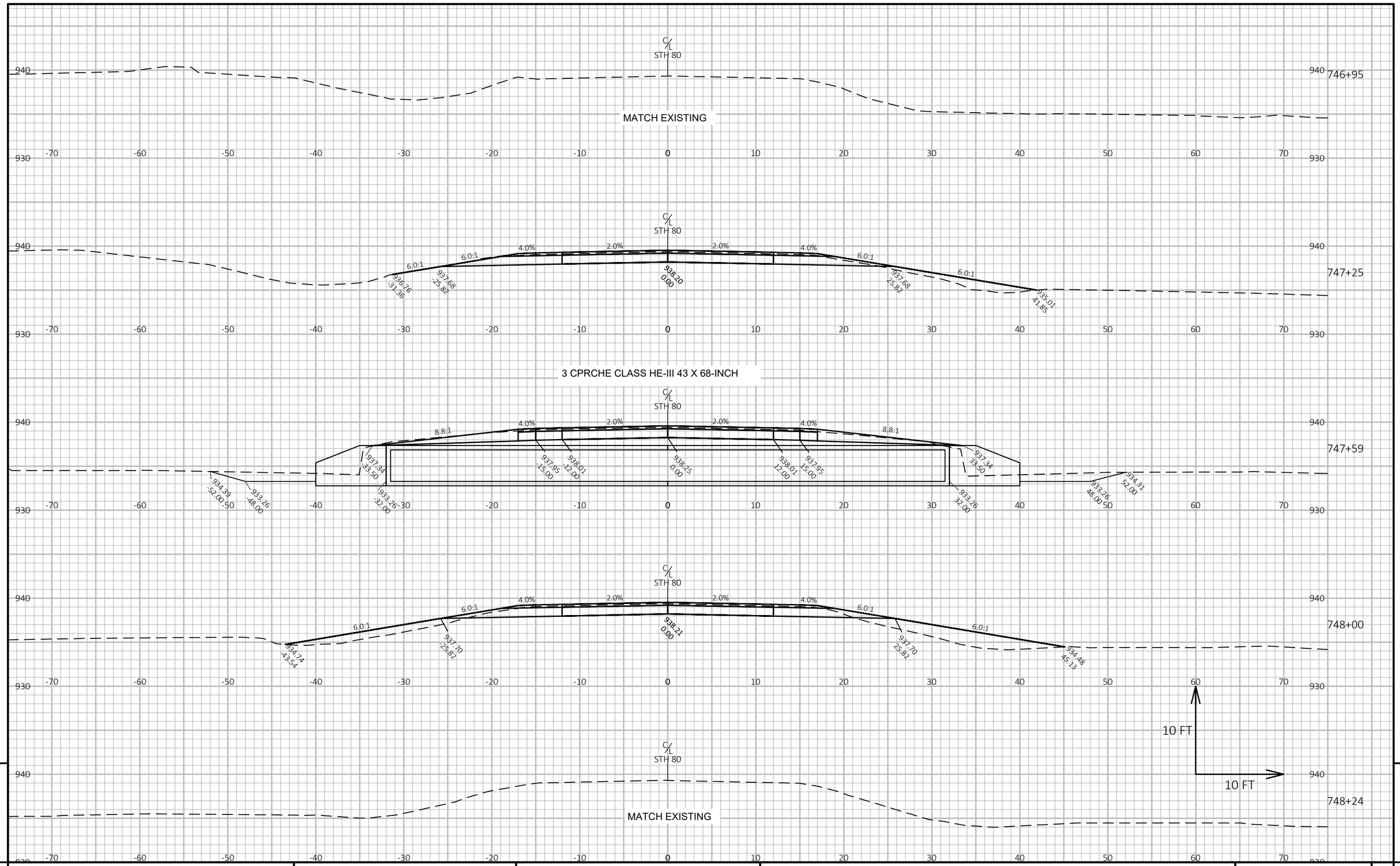
PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	CROSS SECTIONS: STH 80	SHEET 140	E

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\090201_XS.DWG
LAYOUT NAME - 27

PLOT DATE : 5/31/2022 10:05 AM PLOT BY : MARZOLF, ROBBI PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49



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PROJECT NO: 1620-02-78	HWY: STH 80	COUNTY: JUNEAU	CROSS SECTIONS: CULVERT REPLACEMENT	SHEET 141	E

FILE NAME : W:\7607\CAD\! 1620-02-08\SHEETSPLAN\090202_XS.DWG
LAYOUT NAME - 1

PLOT DATE : 5/31/2022 10:23 AM PLOT BY : MARZOLF, ROBBI PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADDS SHEET 49

Wisconsin Department of Transportation

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