

PROJECT ID: 3070-04-60
WITH: N/A

COUNTY: DANE

ORDER OF SHEETS

Section No.	1	Title
Section No.	2	Typical Sections and Details
Section No.	3	Estimate of Quantities
Section No.	3	Miscellaneous Quantities
Section No.	4	Right of Way Plat
Section No.	5	Plan and Profile
Section No.	6	Standard Detail Drawings
Section No.	7	Sign Plates
Section No.	8	Structure Plans
Section No.	9	Computer Earthwork Data
Section No.	9	Cross Sections

TOTAL SHEETS = 200



04

DESIGN DESIGNATION 3070-04-30

A.A.D.T.	2023	=	5,530
A.A.D.T.	2043	=	6,360
D.H.V.		=	379
D.D.		=	50/50
T.		=	13.3%
DESIGN SPEED		=	25-55 MPH
ESALS		=	1,600,000

CONVENTIONAL SYMBOLS

PLAN	
CORPORATE LIMITS	
PROPERTY LINE	
LOT LINE	
LIMITED HIGHWAY EASEMENT	
EXISTING RIGHT OF WAY	
PROPOSED OR NEW R/W LINE	
SLOPE INTERCEPT	
REFERENCE LINE	
EXISTING CULVERT	
PROPOSED CULVERT (Box or Pipe)	
COMBUSTIBLE FLUIDS	
MARSH AREA	
WOODED OR SHRUB AREA	

PROFILE

GRADE LINE	
ORIGINAL GROUND	
MARSH OR ROCK PROFILE (To be noted as such)	
SPECIAL DITCH	
GRADE ELEVATION	
CULVERT (Profile View)	
UTILITIES	
ELECTRIC	
FIBER OPTIC	
GAS	
SANITARY SEWER	
STORM SEWER	
TELEPHONE	
WATER	
UTILITY PEDESTAL	
POWER POLE	
TELEPHONE POLE	

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

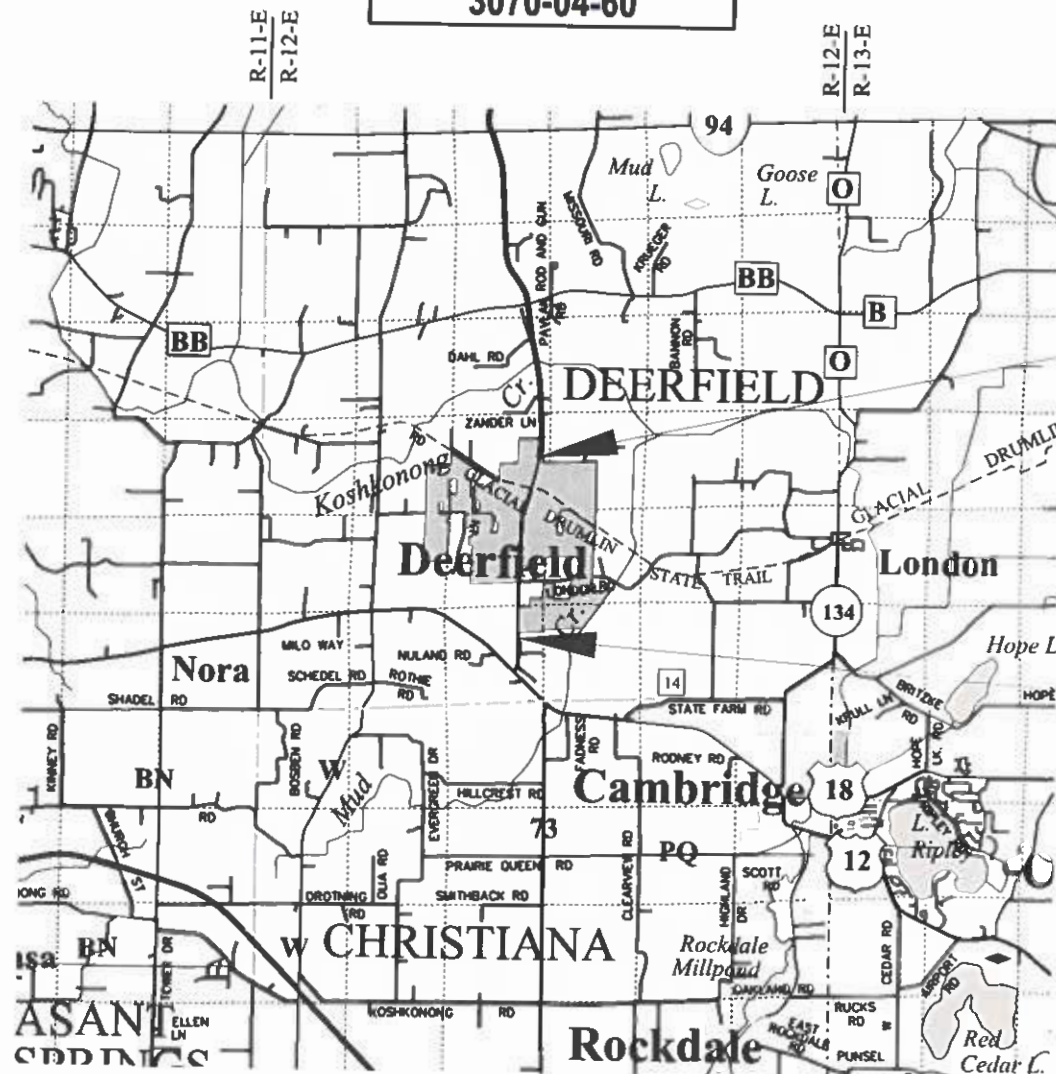
139 - COLUMBUS

SHAUL LANE TO NORTH STREET

STH 73

DANE COUNTY

STATE PROJECT NUMBER
3070-04-60



T-8-N
T-7-N

END PROJECT
STA 125+83.00

BEGIN PROJECT
STA 32+73.54
Y=468,523.825
X=903,270.033

LAYOUT
SCALE 0 2 MI

TOTAL NET LENGTH OF CENTERLINE = 1.763 MI

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COUNTY COORDINATES, DANE COUNTY, NAD83 (2011), IN U.S. SURVEY FEET.

VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCE S. GRID DISTANCE MAY BE USED AS GROUND DISTANCES. ELEVATION SHOWN ON THIS PLAN ARE REFERENCE TO THE NORTH AMERICAN VERTICAL DATUM OF 1988, NAVD88 (2012).

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
3070-04-60	WISC 2023138	1

emcs inc

1600 Aspen Commons, Suite 230
Middleton, WI 53562
608.827.8810 Fax 608.833.3198



7/26/22 Cory T. Inman
(Date) (Signature)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY	
Surveyor	EMCS, INC
Designer	EMCS, INC
Project Manager	AMY COUGHLIN
Regional Examiner	SW REGION
Regional Supervisor	ALEX HAGEN

APPROVED FOR THE DEPARTMENT
DATE: 7/26/2022 Amy Coughlin
(Signature)

ORDER OF SECTION 2 SHEETS

- GENERAL NOTES
- PROJECT OVERVIEW
- TYPICAL SECTIONS
- CONSTRUCTION DETAILS
- PAVING DETAILS - CURB RAMPS
- STORM SEWER
- PERMANENT SIGNING AND MARKING
- TRAFFIC CONTROL
- DETOUR PLAN

GENERAL NOTES

- THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.
- THE CONTRACTOR SHALL NOTIFY DIGGERS HOTLINE AND AFFECTED UTILITIES PRIOR TO THE START OF WORK. ANY LOCAL MUNICIPAL UTILITY WHICH IS NOT A MEMBER OF THE DIGGERS HOTLINE MUST BE CONTACTED SEPARATELY.
- PRIOR TO THE PLACEMENT OF GUARDRAIL, THE SHOULDERS SHALL BE IN PLACE, SHAPED AND COMPACTED.
- THE CONTRACTOR'S PAVING OPERATIONS SHALL BE CONSISTENT WITH THE PLAN TYPICAL SECTIONS AND CONSTRUCTED TO PREVENT HMA LONGITUDINAL JOINTS FROM BEING LOCATED WITHIN A DRIVING, TURNING, BIKE OR PARKING LANE.
- CONTRACTOR WILL BE RESPONSIBLE FOR RESHAPING AND SEEDING ANY PREVIOUSLY GRASSED AREAS WHICH ARE DISTURBED BY OPERATIONS OUTSIDE OF THE NORMAL CONSTRUCTION LIMITS.
- TOPSOIL SHALL BE PLACED 1 INCH BELOW THE TOP OF ADJACENT CONCRETE CURBS OR SIDEWALKS.
- THE QUANTITY OF THE ITEMS FOR EROSION PROTECTION INCLUDES AN UNDISTRIBUTED AMOUNT FOR PROTECTION, CONTROL AND ABATEMENT OF WATER POLLUTION RESULTING FROM SOIL EROSION. THE DISTRIBUTION AND LOCATION OF THESE MATERIALS ARE TO BE DETERMINED BY THE ENGINEER.
- DISTURBED AREAS WITHIN THE RIGHT OF WAY ARE TO BE SODDED AS DIRECTED BY THE ENGINEER.
- A CONVERSION FACTOR OF 2.10 TONS/CY IS USED TO ESTIMATE QUANTITIES FOR BASE AGGREGATE DENSE 3/4-INCH.
- A CONVERSION FACTOR OF 2.00 TONS/CY IS USED TO ESTIMATE QUANTITIES FOR BASE AGGREGATE DENSE 1 1/4-INCH AND BASE AGGREGATE DENSE 3-INCH.
- A CONVERSION FACTOR OF 112 LB/SY/IN. IS USED TO ESTIMATE QUANTITIES FOR HMA PAVEMENT.
- APPLY TACK COAT AT A RATE OF 0.07 GAL/SY TO MILLED SURFACES.
- APPLY TACK COAT AT A RATE OF 0.05 GAL/SY BETWEEN LAYERS OF HMA PAVEMENT.
- ASPHALTIC PAVEMENT SHALL BE CONSTRUCTED WITH THE FOLLOWING LAYERS AND GRADATIONS:

TOTAL LAYER PAVEMENT THICKNESS	LAYERS	HMA PAVEMENT ITEM
4.00"	1.75" (UPPER) 2.25" (LOWER)	4 MT 58-28 S (UPPER) 3 MT 58-28 S (LOWER)
1.75"	1.75" (UPPER)	4 MT 58-28 S (UPPER)
6.00"	1.75" (UPPER) 2.25" (MIDDLE) 2.00" (LOWER)	4 MT 58-28 S (UPPER) 3 MT 58-28 S (MIDDLE) ASPHALTIC SURFACE (LOWER)

UTILITY CONTACTS

COMMUNICATIONS

SPECTRUM COMMUNICATIONS
DAVID MOLDENHAUER
1348 PLAINFIELD AVENUE
JANESVILLE, WI 53545
PHONE: (608)206-0494
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CENTURYLINK
JASON BUSSE
3235 INTERTECH DR, SUITE 600
BROOKFIELD, WI 53045
PHONE: (715)415-3214
EMAIL: JASON.BUSSE@LUMEN.COM

FRONTIER COMMUNICATIONS OF WI LLC
RUSS RYAN
315 OAK STREET
OAKFIELD, WI 53065
PHONE: (920)583-3275
EMAIL: RUSSELL.W.RYAN@FTR.COM

SPRINT COMMUNICATIONS CO LP
DAN HILLIARD
PHONE: (612)217-3526
EMAIL: DAN.J.HILLIARD@T-MOBILE.COM

ELECTRIC

ALLIANT ENERGY - ELECTRIC
DIRK HUDSON
5512 STATE ROAD 19
WAUNAKEE, WI 53597
PHONE: (608)279-0798
EMAIL: DHUDSON@INTERCON-CONST.COM

WE ENERGIES - ELECTRIC
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500 S 116TH STREET
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DEERFIELD PUBLIC WORKS - ELECTRIC
JOHN DOYLE
205 NORTH INDUSTRIAL PARK DRIVE
DEERFIELD, WI 53531
PHONE: (608)764-5497
EMAIL: JDOYLE@DEERFIELDWI.COM

GAS/PETROLEUM

ALLIANT ENERGY - GAS/PETROLEUM
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EMAIL: DHUDSON@INTERCON-CONST.COM

SEWER

DEERFIELD WATER UTILILITY - SEWER
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205 NORTH INDUSTRIAL PARK DRIVE
DEERFIELD, WI 53531
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EMAIL: JDOYLE@DEERFIELDWI.COM

WATER

DEERFIELD WATER UTILILITY - WATER
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205 NORTH INDUSTRIAL PARK DRIVE
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PHONE: (608)764-5497
EMAL: JDOYLE@DEERFIELDWI.COM



OTHER AGENCIES

WDNR LIASON

DNR SOUTH CENTRAL REGION HEADQUARTERS
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WISCONSIN HEIGHT MODERNIZATION PROGRAM

JACOB ROCKWEILER, P.E.
PHONE: 608-516-6362
EMAIL: JACOB.ROCKWEILER@DOT.WI.GOV

WISDOT DESIGN

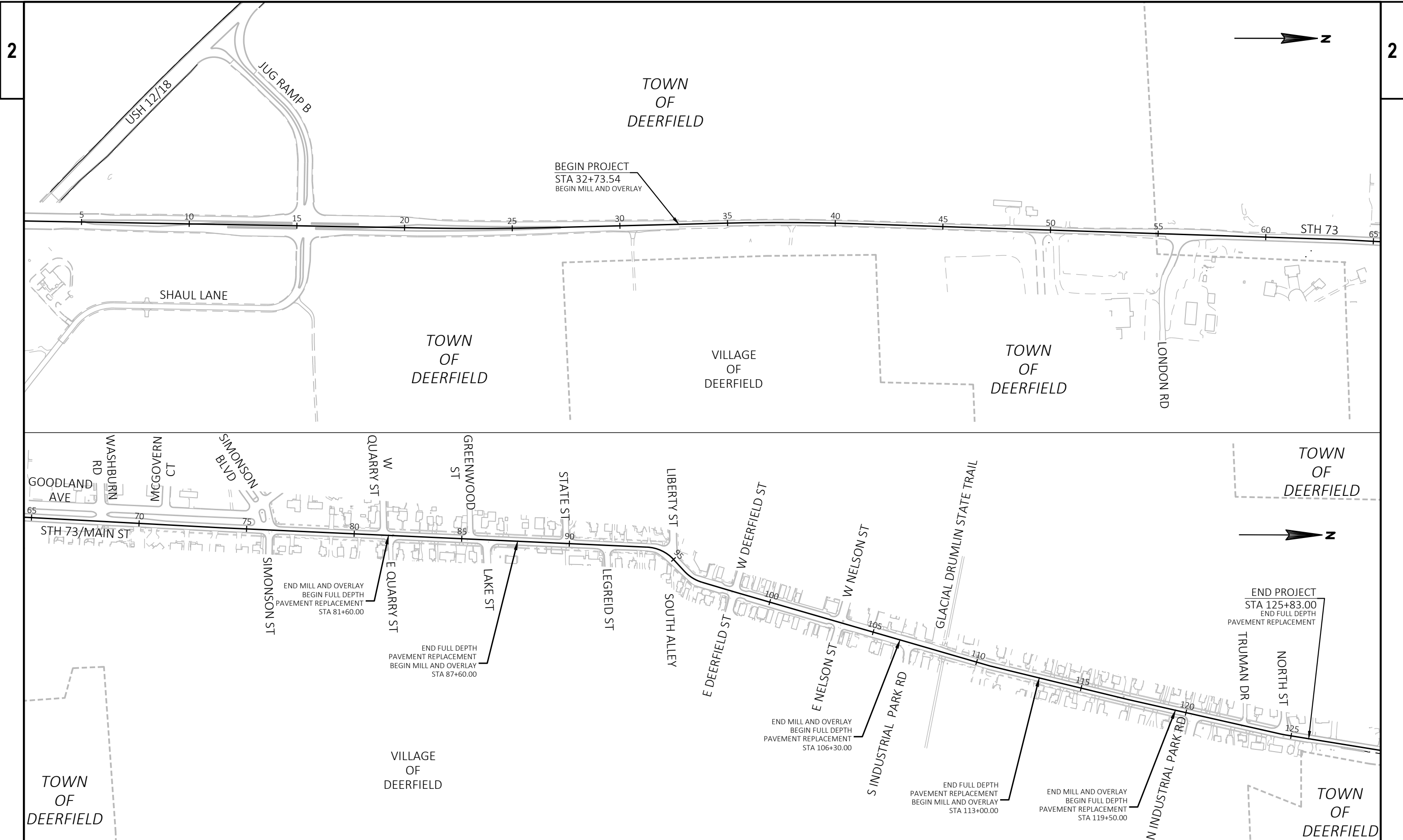
PROJECT MANAGER
WISDOT SW REGION
AMY COUGHLIN
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CONSULTANT DESIGN

EMCS, INC.
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MIDDLETON, WI 53562
PHONE: (608) 665-9819
EMAIL: CINMAN@EMCSINC.COM

STANDARD ABBREVIATIONS

AGG	AGGREGATE	MEF	MAXIMUM EXTEND FEASIBLE
<	ANGLE	MGAL	1000 GALLONS
AE,AEW	APRON ENDWALL	N	NORTH
ASPH.	ASPHALTIC	NB	NORTHBOUND
A.D.T.	AVERAGE DAILY TRAFFIC	NOR.	NORMAL
A.A.D.T.	ANNUAL AVERAGE DAILY TRAFFIC	PAV'T	PAVEMENT
C/L	CENTER LINE	P.L.E.	PERMANENT LIMITED EASEMENT
Δ	CENTRAL ANGLE OR DELTA	P.C.	POINT OF CURVATURE
CMCP	CORRUGATED METAL CULVERT PIPE	P.I.	POINT OF INTERSECTION
CMP	CORRUGATED METAL PIPE	P.T.	POINT OF TANGENCY
CO.	COUNTY	P.E.	PRIVATE ENTRANCE
CTH	COUNTY TRUNK HIGHWAY	R	RADIUS OR RANGE
CR.	CREEK	R/L	REFERENCE LINE
CY	CUBIC YARD	R.C.C.P.	REINFORCED CONCRETE CULVERT PIPE
C&G	CURB AND GUTTER	REQ'D	REQUIRED
D	DEGREE OF CURVE	RT	RIGHT
D.H.V.	DESIGN HOURLY VOLUME	R/W	RIGHT OF WAY
D.D.	DIRECTIONAL DISTRIBUTION	RD.	ROAD
EA	EACH	SHLD.	SHOULDER(S)
E	EAST	S	SOUTH
EB	EASTBOUND	SB	SOUTHBOUND
ELEC.	ELECTRIC(AL), ELEC. CABLE	S.F.	SQUARE FOOT (FEET)
EL., ELEV.	ELEVATION	SDD	STANDARD DETAIL DRAWING(S)
ESALS	EQUIVALENT SINGLE AXLE LOADS	STH	STATE TRUNK HIGHWAY
EXC.	EXCAVATION	STA.	STATION
EXIST	EXISTING	S.E.	SUPERELEVATION
F.E.	FIELD ENTRANCE	S.I.	SLOPE INTERCEPT
CWT	HUNDRED WEIGHT	T.	PERCENT TRUCKS
IH	INTERSTATE HIGHWAY	TEL.	TELEPHONE
LT	LEFT	T.L.E.	TEMPORARY LIMITED EASEMENT
L.	LENGTH OF CURVE	TYP	TYPICAL
L.F.	LINEAR FOOT(FEET)	VAR	VARIABLE
LC.	LONG CHORD	W	WEST
LS	LUMP SUM	WB	WESTBOUND



PROJECT NO: 3070-04-60	HWY: STH 73	COUNTY: DANE	PROJECT OVERVIEW	SHEET	E
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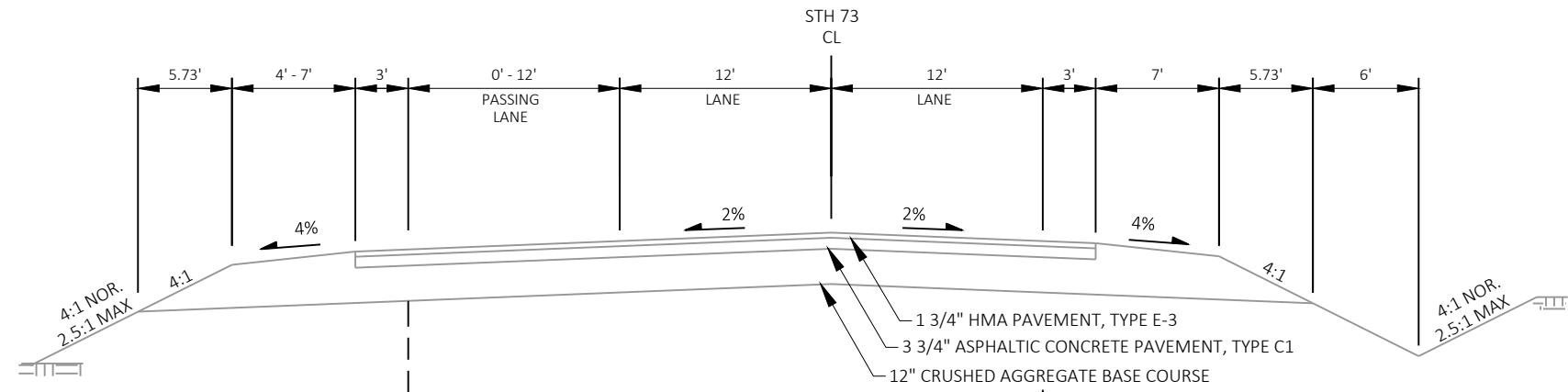
PLOT DATE : 7/21/2022 8:57 AM

PLOT BY : RYAN SCHUPPIE

PLOT NAME :

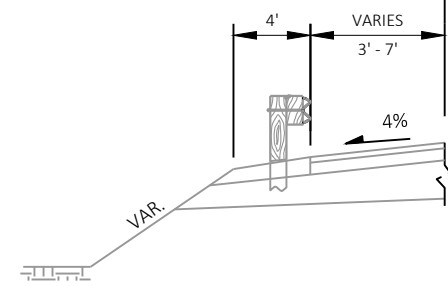
PLOT SCALE : 1 IN=400 FT

WISDOT/CADD SHEET 42



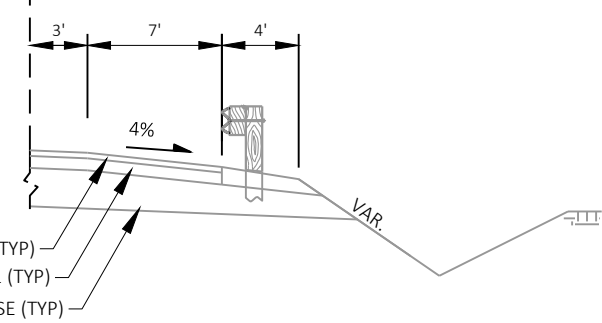
TYPICAL EXISTING SECTION

STH 73
STA 32+74 - STA 61+06



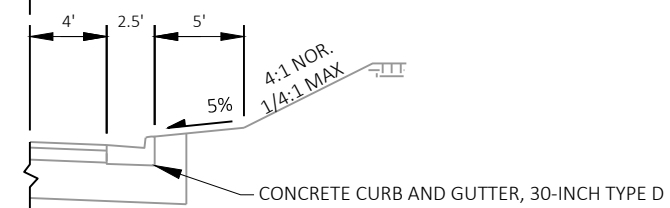
TYPICAL EXISTING SECTION

STH 73
STA 55+28 - STA 61+06



TYPICAL EXISTING SECTION

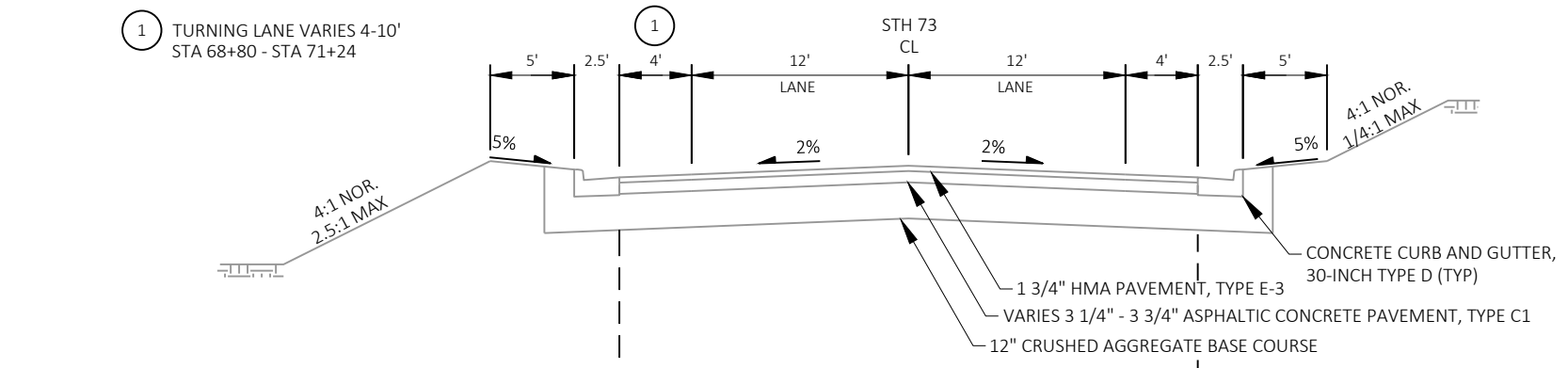
STH 73
STA 32+74 - STA 34+91



TYPICAL EXISTING SECTION

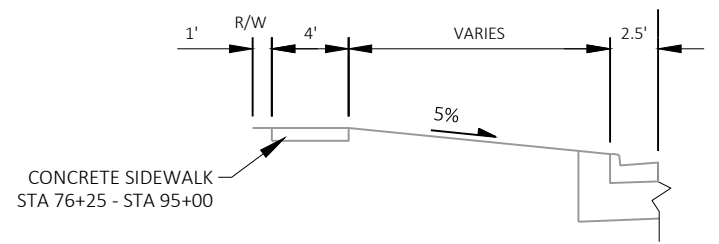
STH 73
STA 55+37 - STA 61+06

1 TURNING LANE VARIES 4-10'
STA 68+80 - STA 71+24



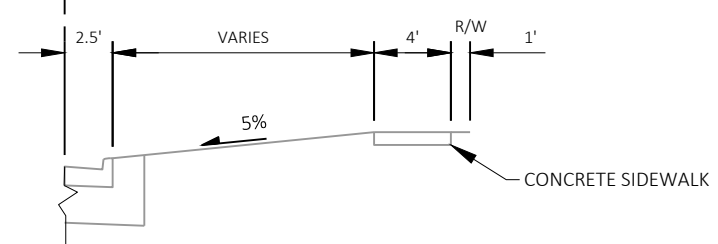
TYPICAL EXISTING SECTION

STH 73
STA 61+06 - STA 95+00
STA 108+90 - STA 125+83



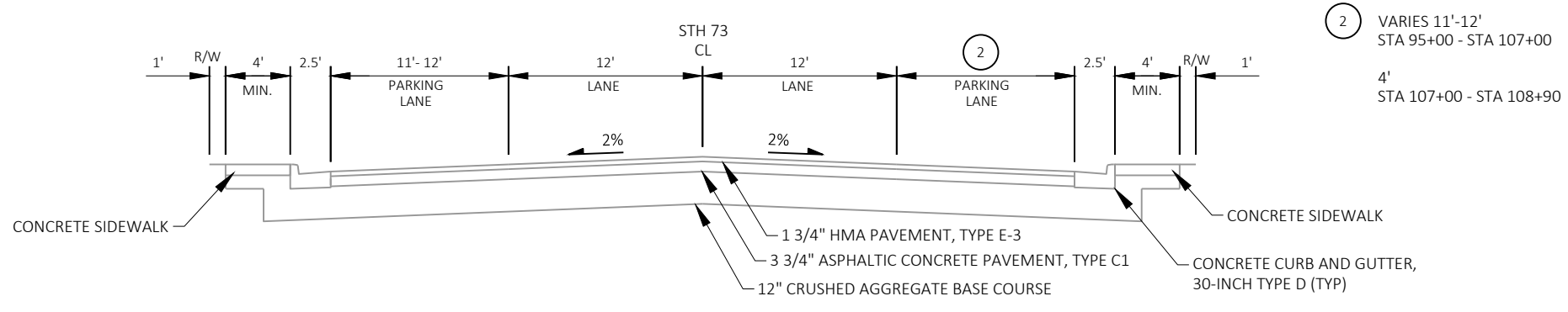
TYPICAL EXISTING SECTION

STH 73
STA 71+24 - STA 95+00



TYPICAL EXISTING SECTION

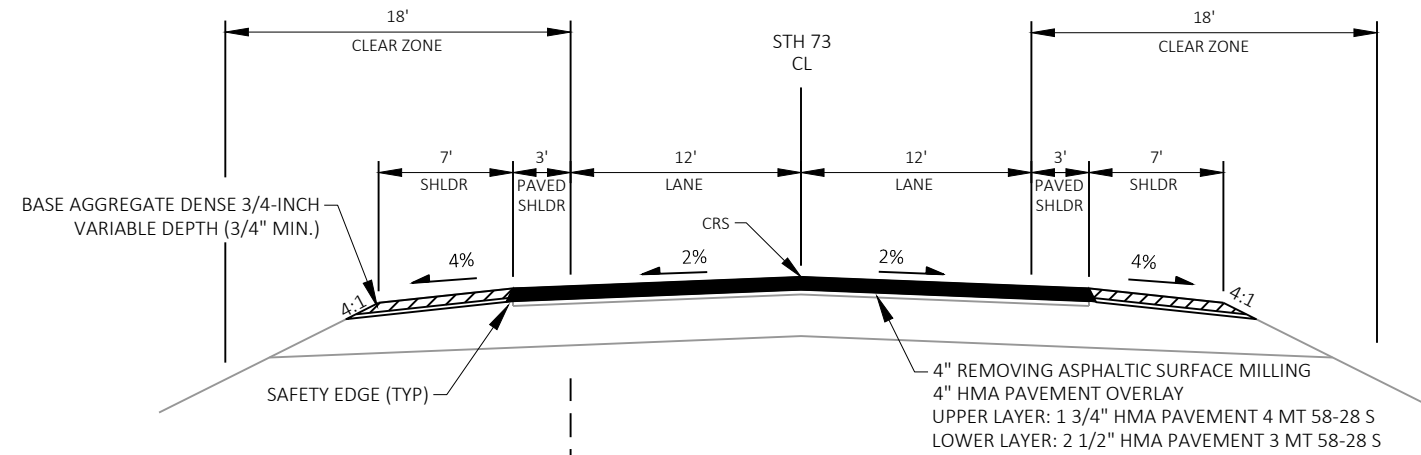
STH 73
STA 68+30 - STA 95+00
STA 108+90 - STA 123+89



TYPICAL EXISTING SECTION

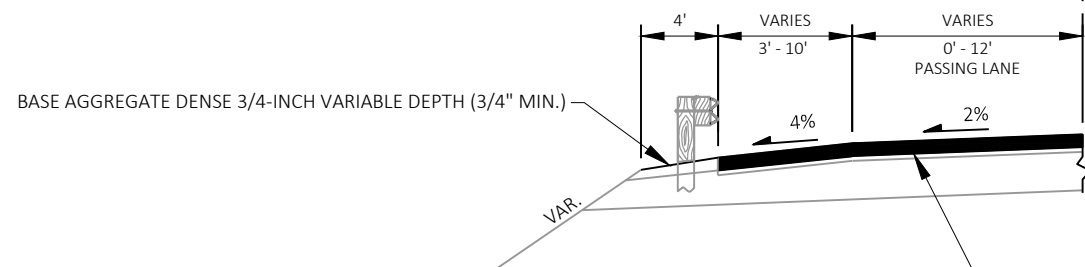
STH 73
STA 95+00 - STA 108+90

2 VARIES 11'-12'
STA 95+00 - STA 107+00
4'
STA 107+00 - STA 108+90



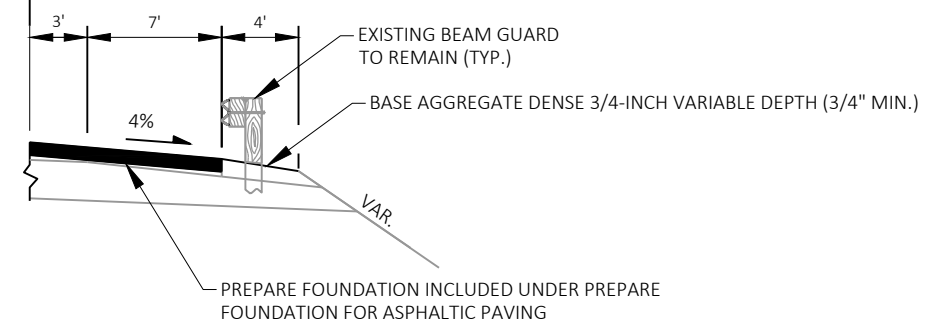
TYPICAL FINISHED SECTION

MILL AND OVERLAY
STH 73
STA 32+74 - STA 61+06



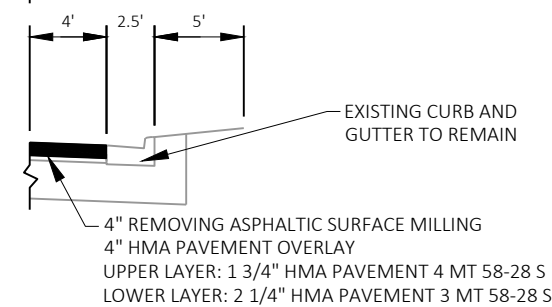
TYPICAL FINISHED SECTION

STH 73
STA 55+03 - STA 60+99



TYPICAL FINISHED SECTION

STH 73
STA 32+74 - STA 34+91

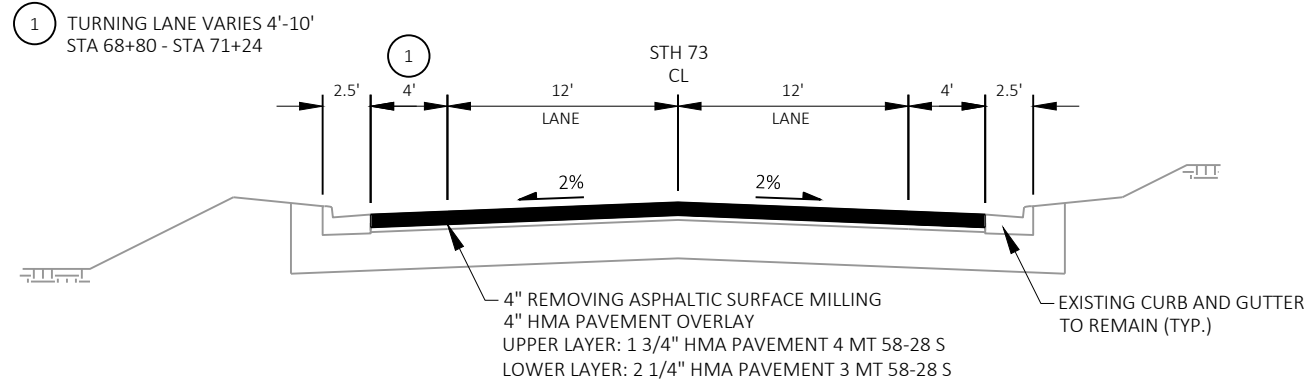


TYPICAL FINISHED SECTION

STH 73
STA 55+37 - STA 61+06

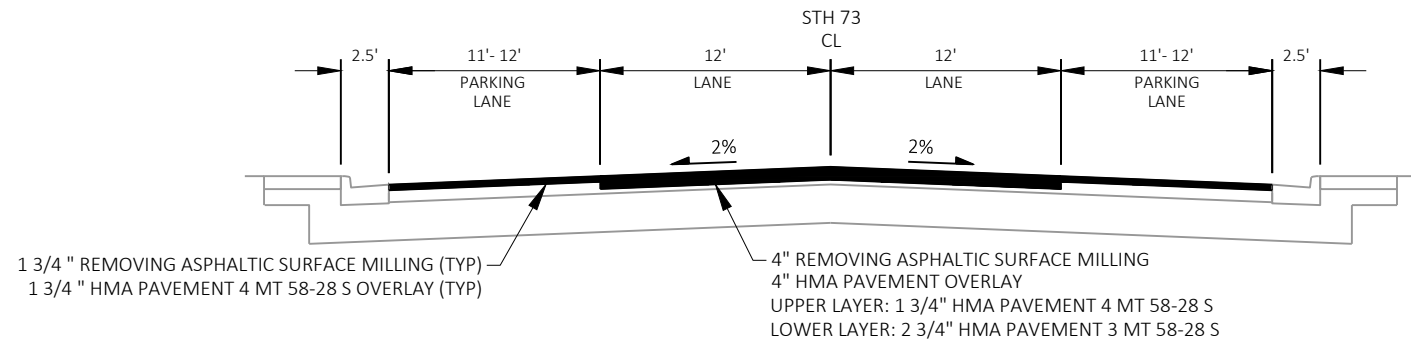
LEGEND

CRS = ASPHALTIC CENTERLINE RUMBLE STRIPS 2-LANE RURAL



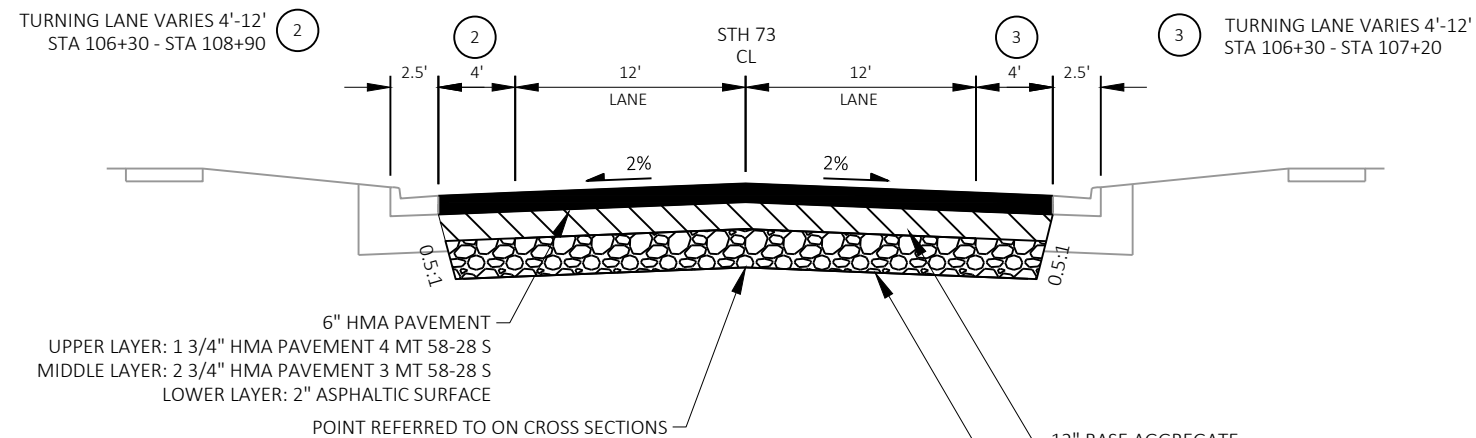
TYPICAL FINISHED SECTION

MILL AND OVERLAY
 STH 73
 STA 61+06 - STA 81+60
 STA 87+60 - STA 95+00
 STA 113+00 - STA 119+50



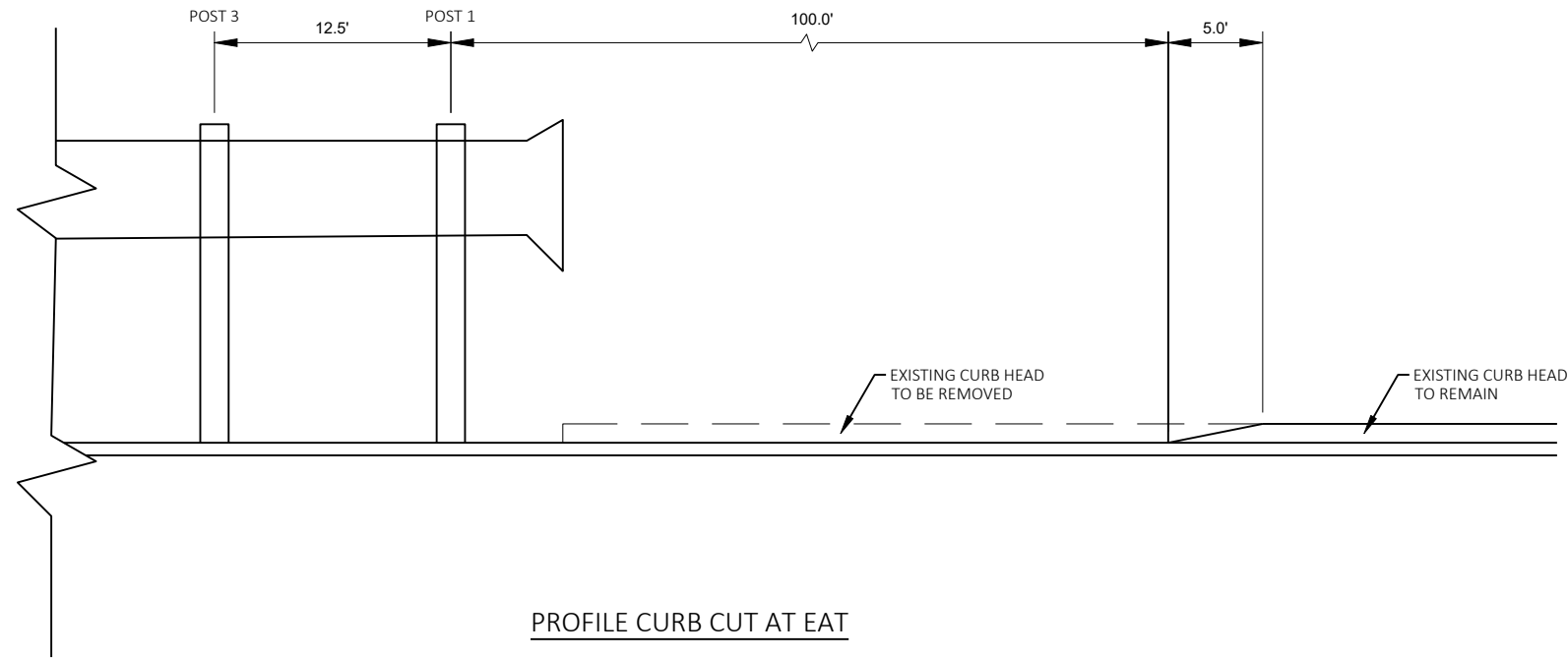
TYPICAL FINISHED SECTION

MILL AND OVERLAY
 STH 73
 STA 95+00 - STA 106+30



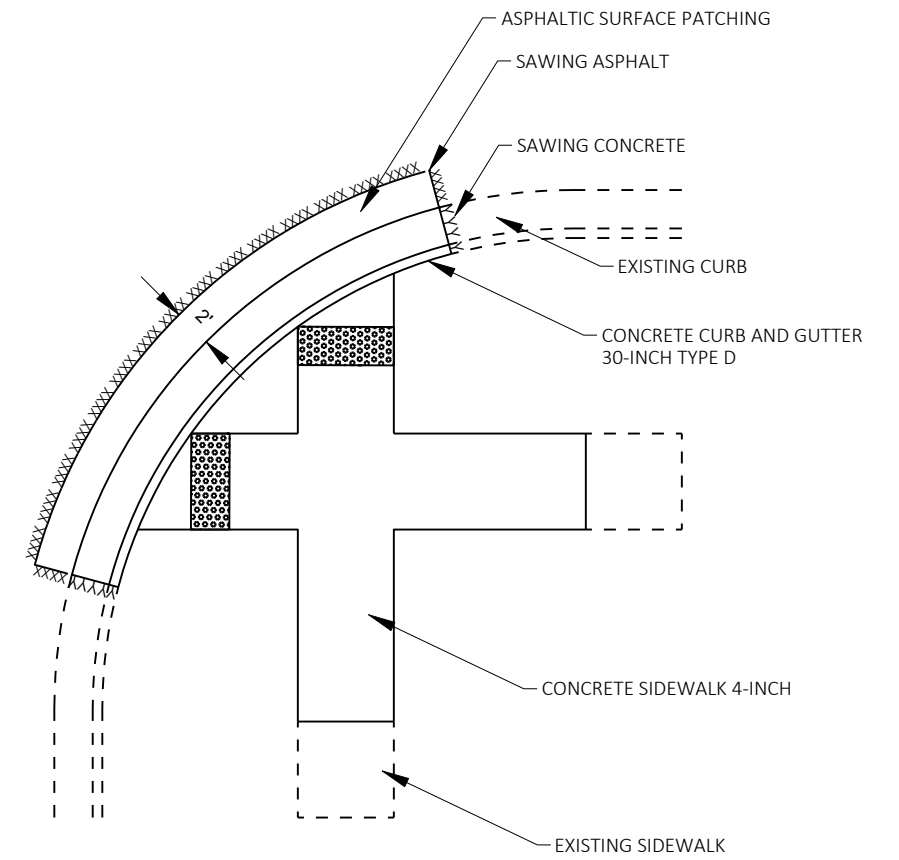
TYPICAL FINISHED SECTION

FULL DEPTH PAVEMENT REPLACEMENT
 STH 73
 STA 81+60 - STA 87+60
 STA 106+30 - STA 113+00
 STA 119+50 - STA 125+83

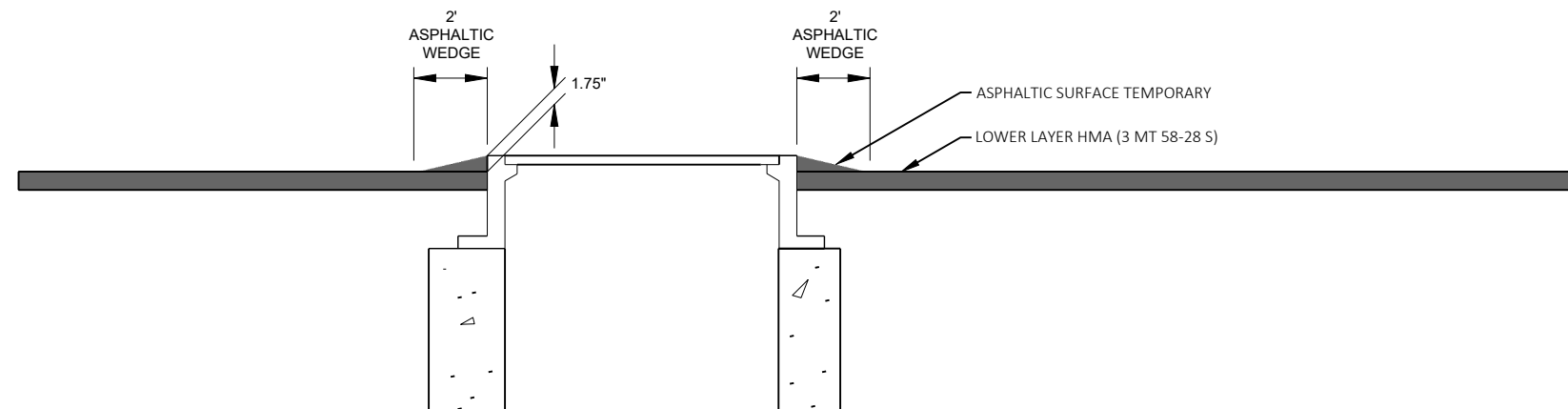


PROFILE CURB CUT AT EAT

STA 61+06 - STA 62+11



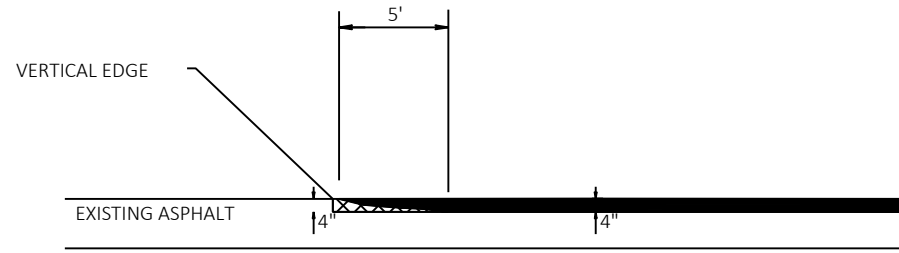
ASPHALT REMOVAL AT CURB RAMPS





TEMPORARY WEDGES DETAIL (MANHOLES)

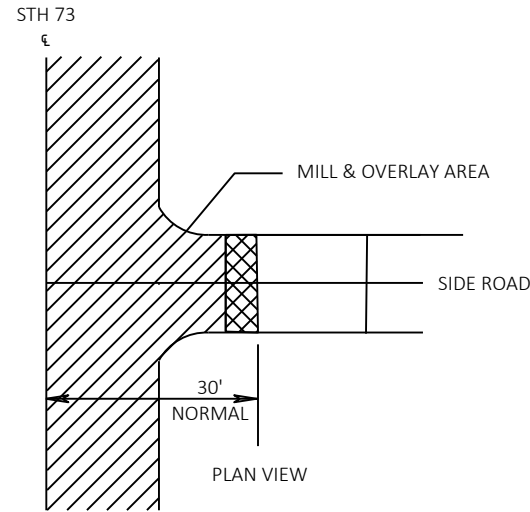
NOTES

PROVIDE ASPHALTIC WEDGES PRIOR TO OPENING LANE TO TRAFFIC
 REMOVAL OF ASPHALTIC WEDGES WILL BE PAID FOR AS "REMOVING ASPHALTIC SURFACE BUTT JOINTS"

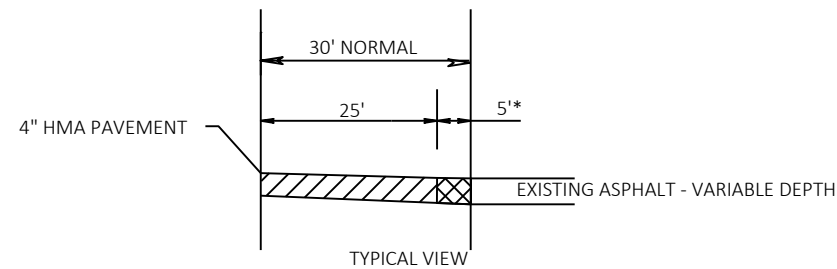


STH 73 DETAIL
REQUIRED AT BEGINNING OF PROJECT

-  REMOVE ASPHALTIC SURFACE WEDGE AT BUTT JOINT TO CREATE VERTICAL EDGE PAID UNDER BID ITEM REMOVING ASPHALTIC SURFACE BUTT JOINTS
-  REMOVING ASPHALTIC SURFACE MILLING

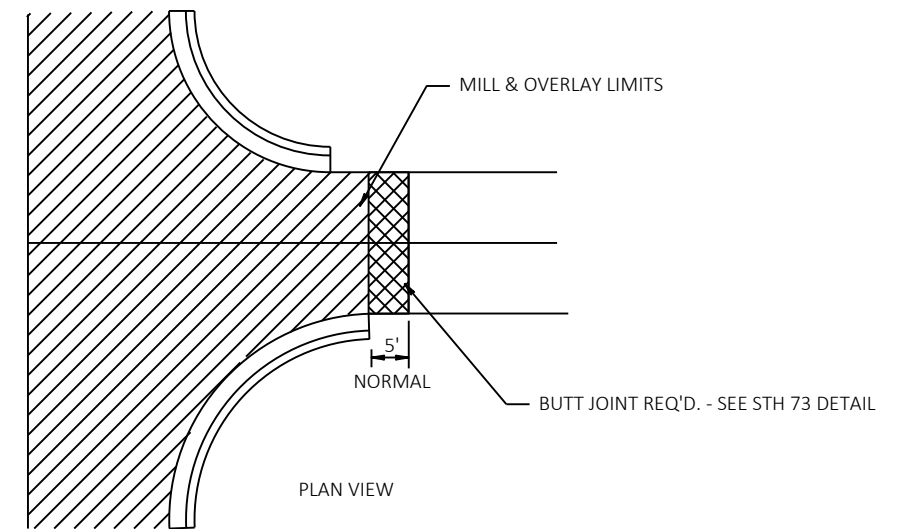


PLAN VIEW





TYPICAL VIEW

SIDE ROAD - NO CURB & GUTTER



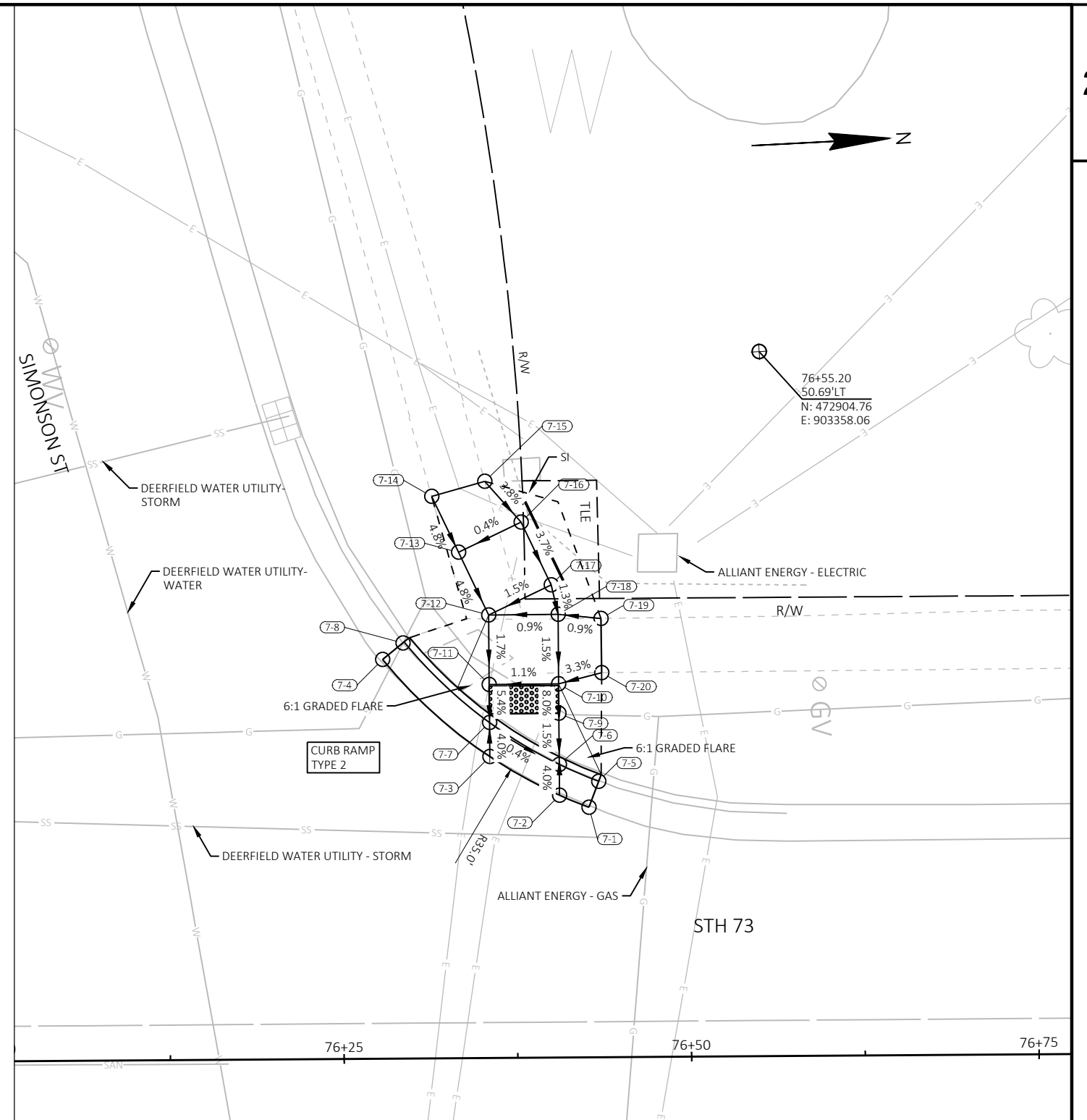
PLAN VIEW

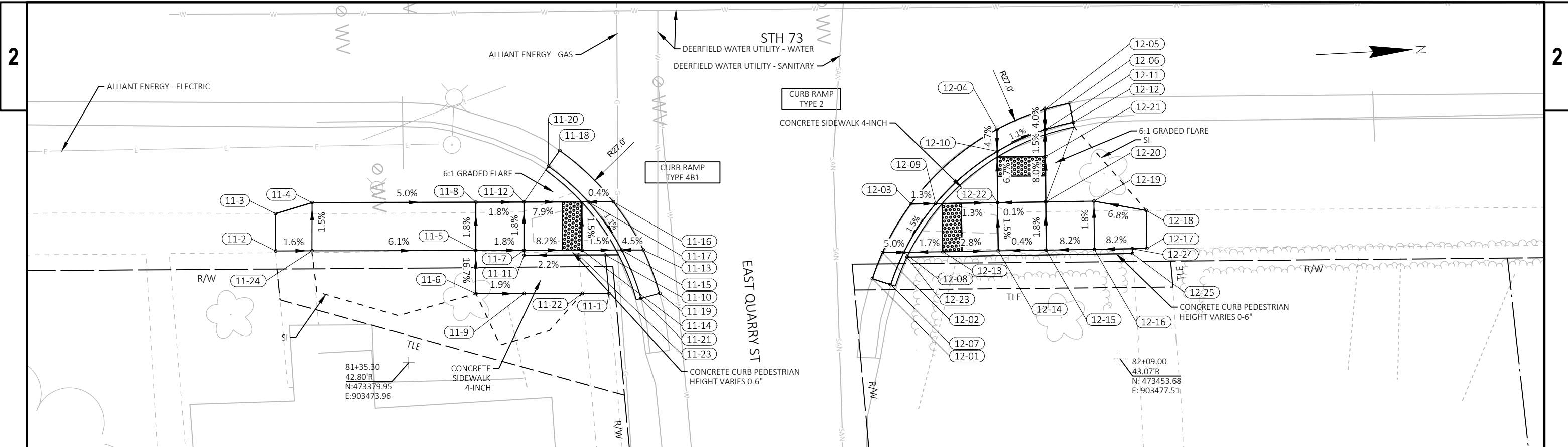
SIDE ROAD - CURB & GUTTER TO REMAIN

-  REMOVE MATERIAL UNDER ITEM 'REMOVING ASPHALT SURFACE MILLING '
 -  REMOVE MATERIAL UNDER ITEM 'REMOVING ASPHALT SURFACE , BUTT JOINTS' MATERIAL SHALL NOT BE REMOVED UNDER THIS ITEM UNTIL 24 HOURS BEFORE SIDE ROAD PAVING.
- SIDE ROAD PAVEMENT DEPTH SHALL MATCH AT MAINLINE PAVEMENT EDGE AND BE TAPERED TO 2" MINIMUM AT JOINT
- SAWING ASPHALT IS INCIDENTAL TO REMOVING ASPHALT SURFACE BUTT JOINT ITEM
- *REMOVING ASPHALTIC SURFACE BUTT JOINT LENGTH EQUALS 5- FEET ON SIDE ROADS

BUTT JOINT DETAILS

Simonson NW					
POINT	STA	OFFSET	ELEV.	NORTHING	EASTING
7-1	76+42.73	17.98' LT	874.54	472890.77	903390.14
7-2	76+40.62	18.87' LT	874.56	472888.70	903389.16
7-3	76+35.62	21.68' LT	874.59	472883.84	903386.12
7-4	76+27.95	28.72' LT	874.69	472876.51	903378.73
7-5	76+43.44	19.85' LT	874.41	472891.57	903388.31
7-6	76+40.62	21.08' LT	874.47	472888.81	903386.95
7-7	76+35.61	24.13' LT	874.49	472883.95	903383.67
7-8	76+29.42	29.90' LT	874.53	472878.04	903377.62
7-9	76+40.61	24.76' LT	874.52	472888.97	903383.28
7-10	76+40.61	26.88' LT	874.69	472889.07	903381.16
7-11	76+35.61	26.87' LT	874.64	472884.07	903380.93
7-12	76+35.60	31.87' LT	874.72	472884.30	903375.94
7-13	76+33.47	36.39' LT	874.97	472882.38	903371.32
7-14	76+31.56	40.42' LT	875.18	472880.67	903367.21
7-15	76+35.39	41.49' LT	875.13	472884.54	903366.33
7-16	76+37.99	38.53' LT	874.99	472886.99	903369.40
7-17	76+40.12	34.01' LT	874.80	472888.92	903374.02
7-18	76+40.60	31.88' LT	874.77	472889.30	903376.17
7-19	76+43.69	31.57' LT	874.80	472892.36	903376.62
7-20	76+43.71	27.67' LT	874.80	472892.20	903380.52



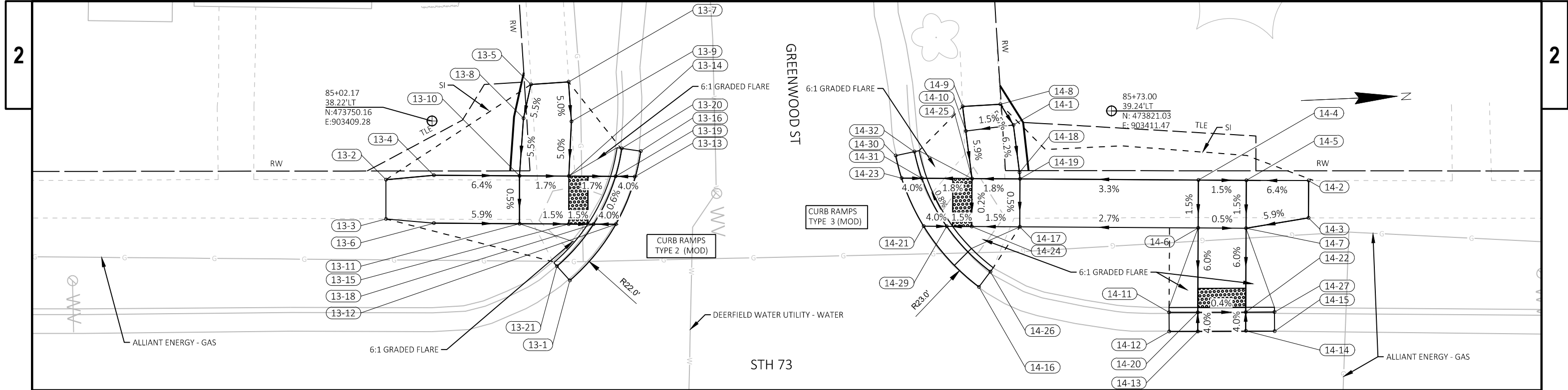


East Quarry SE					
POINT	STA	OFFSET	ELEV	NORTHING	EASTING
11-1	81+56.03	35.81' RT	871.10	473401.08	903467.91
11-2	81+21.54	31.12' RT	872.15	473366.75	903461.65
11-3	81+21.57	27.26' RT	872.10	473366.96	903457.80
11-4	81+25.37	26.14' RT	872.01	473370.81	903456.85
11-5	81+42.27	31.22' RT	871.05	473387.53	903462.71
11-6	81+42.24	35.70' RT	871.79	473387.30	903467.20
11-7	81+47.27	31.25' RT	870.96	473392.53	903462.97
11-8	81+42.30	26.22' RT	870.96	473387.79	903457.72
11-9	81+47.24	35.74' RT	871.70	473392.30	903467.46
11-10	81+55.69	31.81' RT	870.51	473400.91	903463.90
11-11	81+47.27	31.75' RT	871.04	473392.51	903463.47
11-12	81+47.30	26.25' RT	870.87	473392.78	903457.98
11-13	81+59.64	31.33' RT	870.51	473404.88	903463.60
11-14	81+53.27	31.29' RT	870.47	473398.52	903463.28
11-15	81+57.40	31.32' RT	870.41	473402.65	903463.49
11-16	81+56.58	26.31' RT	870.35	473402.05	903458.45

East Quarry SE					
POINT	STA	OFFSET	ELEV	NORTHING	EASTING
11-17	81+53.98	26.30' RT	870.34	473399.45	903458.32
11-18	81+51.05	20.94' RT	870.41	473396.76	903452.84
11-19	81+61.28	35.84' RT	870.62	473406.32	903468.18
11-20	81+49.88	22.56' RT	870.45	473395.52	903454.40
11-21	81+59.35	36.36' RT	870.50	473404.37	903468.61
11-22	81+53.27	35.79' RT	871.29	473398.32	903467.77
11-23	81+52.90	31.79' RT	870.58	473398.13	903463.76
11-24	81+25.33	31.14' RT	872.08	473370.54	903461.85

East Quarry NE					
POINT	STA	OFFSET	ELEV	NORTHING	EASTING
12-01	81+83.39	34.54' RT	870.46	473428.46	903467.86
12-02	81+84.45	31.84' RT	870.38	473429.65	903465.20
12-03	81+87.44	26.82' RT	870.21	473432.86	903460.32
12-04	81+96.46	19.16' RT	869.84	473442.20	903453.07
12-05	82+01.46	17.14' RT	869.76	473447.29	903451.28
12-06	82+03.99	16.54' RT	869.72	473449.84	903450.78
12-07	81+85.28	35.17' RT	870.30	473430.33	903468.57
12-08	81+86.68	31.82' RT	870.25	473431.87	903465.29
12-09	81+90.02	26.80' RT	870.17	473435.43	903460.42
12-10	81+96.45	21.45' RT	869.74	473442.10	903455.36
12-11	82+01.45	19.24' RT	869.68	473447.19	903453.37
12-12	82+04.36	18.50' RT	869.59	473450.13	903452.76
12-13	81+90.72	31.80' RT	870.33	473435.91	903465.44
12-14	81+96.48	31.76' RT	870.17	473441.66	903465.66
12-15	82+01.44	31.73' RT	870.18	473446.62	903465.85
12-16	82+06.44	31.70' RT	870.59	473451.62	903466.04

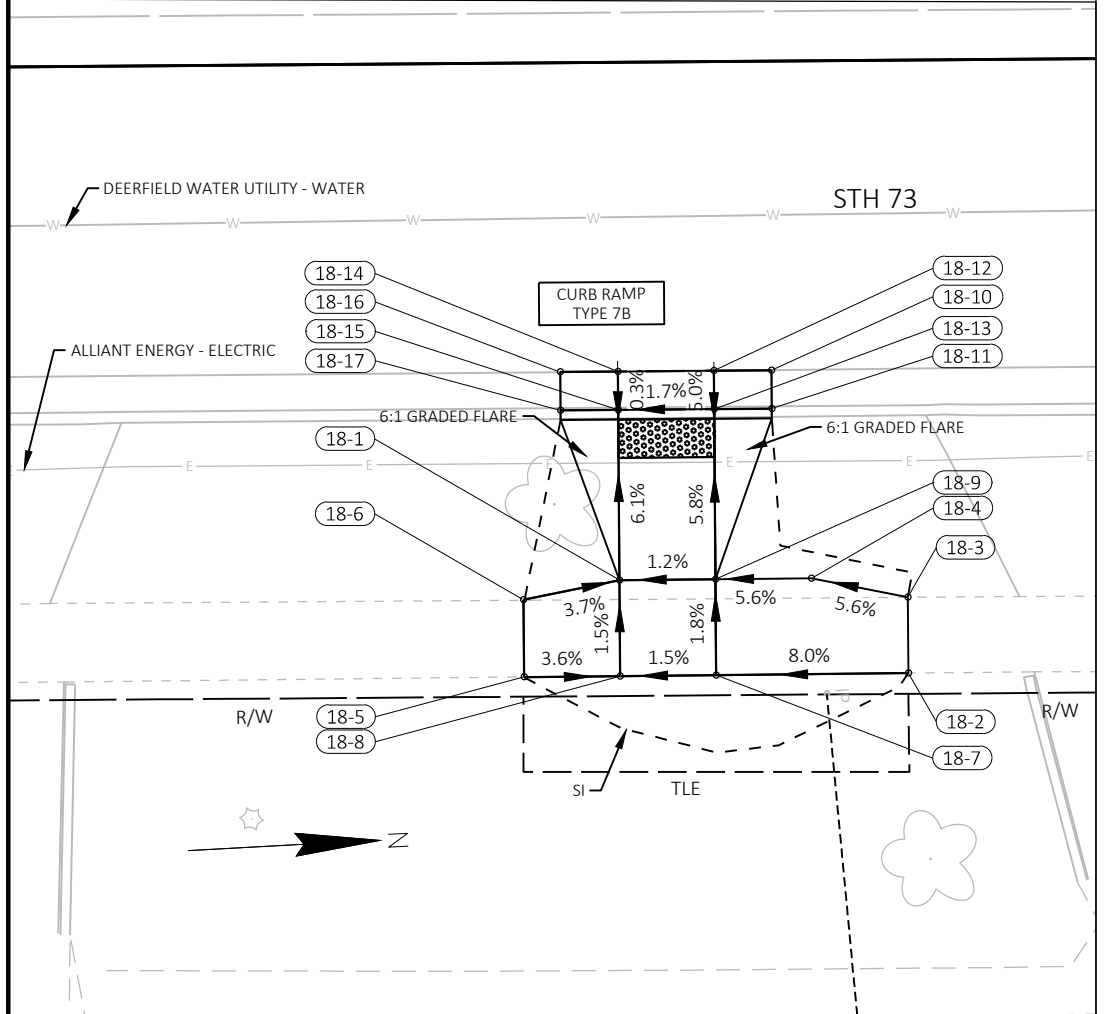
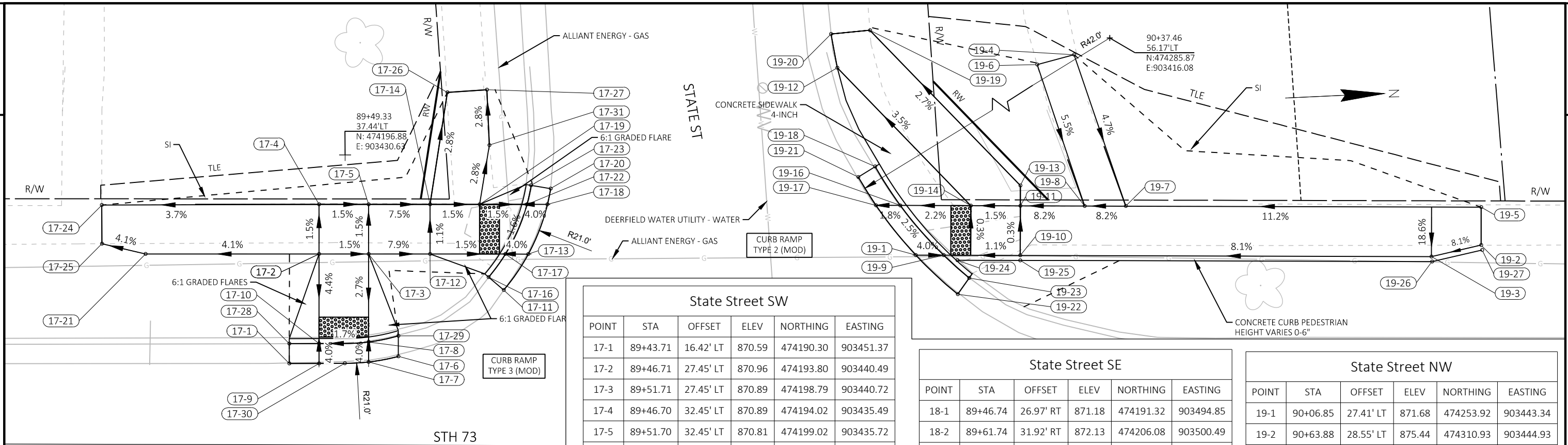
East Quarry NE					
POINT	STA	OFFSET	ELEV	NORTHING	EASTING
12-17	82+11.89	31.67' RT	870.90	473457.07	903466.25
12-18	82+11.88	27.72' RT	870.88	473457.23	903462.30
12-19	82+06.45	26.70' RT	870.50	473451.85	903461.05
12-20	82+01.45	26.73' RT	870.09	473446.85	903460.86
12-21	82+01.45	22.04' RT	869.72	473447.06	903456.16
12-22	81+96.45	26.76' RT	870.09	473441.85	903460.67
12-23	81+86.99	32.32' RT	870.59	473432.16	903465.80
12-24	82+10.37	31.68' RT	870.91	473455.55	903466.19
12-25	82+10.40	32.18' RT	870.93	473455.55	903466.69



Greenwood-SW					
POINT	STA	OFFSET	ELEV	NORTHING	EASTING
13-1	85+16.53	21.55' LT	867.76	473763.77	903426.57
13-2	84+97.37	32.09' LT	868.61	473745.09	903415.19
13-3	84+97.36	28.00' LT	868.52	473744.90	903419.28
13-4	85+02.37	32.55' LT	868.29	473750.10	903414.96
13-5	85+12.51	42.01' LT	868.26	473760.65	903405.95
13-6	85+02.36	27.55' LT	868.22	473749.87	903419.96
13-7	85+16.42	42.28' LT	868.13	473764.57	903405.86
13-8	85+11.72	38.49' LT	868.06	473759.71	903409.44
13-9	85+16.71	38.14' LT	867.92	473764.68	903410.01
13-10	85+11.30	32.48' LT	867.72	473759.02	903415.42
13-11	85+11.28	27.48' LT	867.70	473758.79	903420.42
13-12	85+21.33	27.40' LT	867.68	473768.82	903420.94
13-13	85+23.31	32.37' LT	867.63	473771.02	903416.07
13-14	85+16.45	32.44' LT	867.63	473764.17	903415.69
13-15	85+16.42	27.44' LT	867.62	473763.91	903420.68
13-16	85+23.94	35.04' LT	867.62	473771.77	903413.42
13-18	85+19.01	27.42' LT	867.58	473766.50	903420.82
13-19	85+21.31	32.40' LT	867.55	473769.02	903415.94
13-20	85+21.96	35.33' LT	867.51	473769.80	903413.05
13-21	85+15.19	23.03' LT	867.63	473762.49	903425.03

Greenwood NW					
POINT	STA	OFFSET	ELEV	NORTHING	EASTING
14-1	85+62.76	37.77' LT	867.99	473810.74	903412.47
14-2	85+93.56	31.98' LT	868.66	473841.24	903419.67
14-3	85+93.56	28.09' LT	868.55	473841.05	903423.55
14-4	85+82.06	32.02' LT	868.26	473829.75	903419.09
14-5	85+87.06	31.99' LT	868.24	473834.74	903419.35
14-6	85+82.04	27.02' LT	868.19	473829.50	903424.09
14-7	85+87.04	26.99' LT	868.17	473834.49	903424.35
14-8	85+61.41	39.93' LT	868.13	473809.48	903410.24
14-9	85+57.47	39.68' LT	868.04	473805.54	903410.31
14-10	85+57.80	37.12' LT	867.91	473805.75	903412.88
14-11	85+79.02	18.21' LT	867.72	473826.07	903432.75
14-12	85+79.02	16.21' LT	867.75	473825.99	903434.75
14-13	85+82.01	16.22' LT	867.74	473828.97	903434.87
14-14	85+87.03	16.29' LT	867.72	473833.99	903435.04
14-15	85+90.02	16.26' LT	867.71	473836.98	903435.21
14-16	85+59.17	20.86' LT	867.69	473806.37	903429.19
14-17	85+63.44	27.13' LT	867.68	473810.92	903423.12
14-18	85+63.41	32.81' LT	867.67	473811.15	903417.45
14-19	85+63.44	32.13' LT	867.70	473811.16	903418.12
14-20	85+82.01	18.22' LT	867.66	473829.07	903432.88
14-21	85+53.40	27.19' LT	867.66	473800.90	903422.60

Greenwood NW					
POINT	STA	OFFSET	ELEV	NORTHING	EASTING
14-22	85+87.01	18.24' LT	867.64	473834.06	903433.09
14-23	85+51.10	32.20' LT	867.61	473798.83	903417.49
14-24	85+58.42	27.16' LT	867.60	473805.91	903422.86
14-25	85+58.45	32.16' LT	867.61	473806.17	903417.87
14-26	85+60.37	22.46' LT	867.60	473807.64	903427.65
14-27	85+90.02	18.26' LT	867.69	473837.06	903433.21
14-29	85+55.81	27.18' LT	867.57	473803.30	903422.72
14-30	85+50.48	34.56' LT	867.56	473798.32	903415.10
14-31	85+53.21	32.19' LT	867.52	473800.94	903417.60
14-32	85+52.43	34.97' LT	867.45	473800.29	903414.78



State Street SW

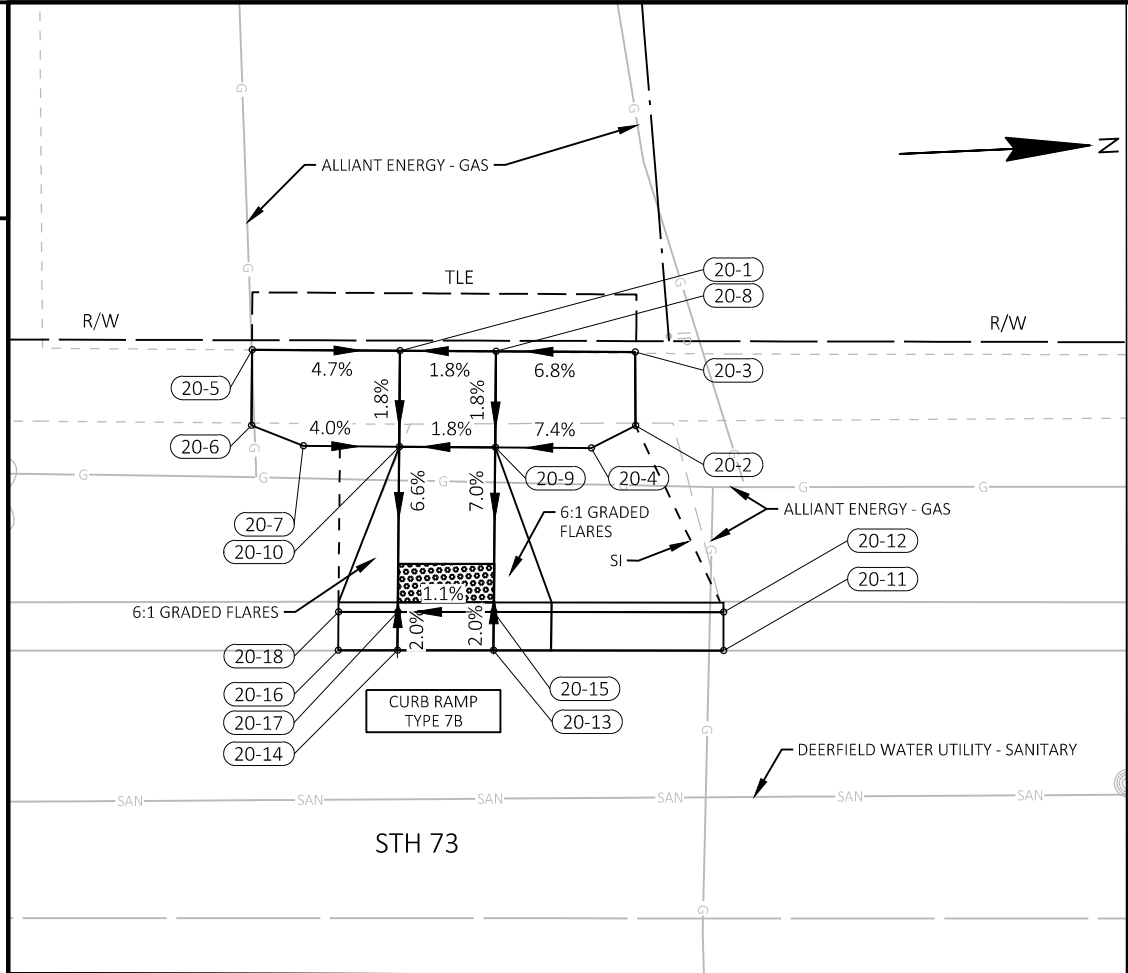
POINT	STA	OFFSET	ELEV	NORTHING	EASTING
17-1	89+43.71	16.42' LT	870.59	474190.30	903451.37
17-2	89+46.71	27.45' LT	870.96	474193.80	903440.49
17-3	89+51.71	27.45' LT	870.89	474198.79	903440.72
17-4	89+46.70	32.45' LT	870.89	474194.02	903435.49
17-5	89+51.70	32.45' LT	870.81	474199.02	903435.72
17-6	89+54.71	17.14' LT	870.78	474201.32	903451.15
17-7	89+51.71	16.58' LT	870.73	474198.30	903451.58
17-8	89+51.71	18.59' LT	870.65	474198.39	903449.57
17-9	89+46.71	16.43' LT	870.64	474193.29	903451.49
17-10	89+46.71	18.43' LT	870.56	474193.39	903449.50
17-11	89+65.33	23.84' LT	870.50	474212.23	903444.96
17-12	89+57.90	27.45' LT	870.40	474204.98	903441.00
17-13	89+67.81	27.46' LT	870.38	474214.87	903441.45
17-14	89+57.90	32.45' LT	870.35	474205.20	903436.01
17-16	89+63.80	25.13' LT	870.30	474210.77	903443.59
17-17	89+65.50	27.46' LT	870.29	474212.57	903441.35
17-18	89+69.73	32.46' LT	870.28	474217.03	903436.55
17-19	89+62.90	32.46' LT	870.27	474210.20	903436.23
17-20	89+70.05	34.05' LT	870.25	474217.42	903434.97
17-21	89+29.21	27.44' LT	870.24	474176.31	903439.69
17-22	89+67.66	32.46' LT	870.20	474214.96	903436.45
17-23	89+68.08	34.38' LT	870.08	474215.47	903434.56
17-24	89+24.78	32.37' LT	870.08	474172.12	903434.56
17-25	89+24.80	28.47' LT	870.06	474171.96	903438.45
17-26	89+59.63	43.73' LT	870.03	474207.46	903424.82
17-27	89+63.62	44.00' LT	869.94	474211.46	903424.73
17-28	89+43.71	18.42' LT	870.50	474190.39	903449.37
17-29	89+54.71	19.22' LT	870.69	474201.41	903449.08
17-30	89+49.31	16.44' LT	870.79	474195.89	903451.61
17-31	89+63.88	38.43' LT	870.10	474211.45	903430.31

State Street SE

POINT	STA	OFFSET	ELEV	NORTHING	EASTING
18-1	89+46.74	26.97' RT	871.18	474191.32	903494.85
18-2	89+61.74	31.92' RT	872.13	474206.08	903500.49
18-3	89+61.74	27.96' RT	871.80	474206.26	903496.53
18-4	89+56.74	26.95' RT	871.52	474201.31	903495.29
18-5	89+41.74	31.97' RT	871.43	474186.10	903499.61
18-6	89+41.74	27.96' RT	871.37	474186.28	903495.61
18-7	89+51.74	31.95' RT	871.33	474196.09	903500.06
18-8	89+46.74	31.97' RT	871.26	474191.09	903499.84
18-9	89+51.74	26.95' RT	871.24	474196.31	903495.06
18-10	89+54.73	16.09' RT	870.95	474199.80	903484.35
18-11	89+54.73	18.09' RT	870.84	474199.71	903486.35
18-12	89+51.73	16.09' RT	870.83	474196.81	903484.22
18-13	89+51.73	18.09' RT	870.73	474196.72	903486.22
18-14	89+46.73	16.10' RT	870.65	474191.81	903483.99
18-15	89+46.73	18.10' RT	870.64	474191.72	903485.99
18-16	89+43.73	16.10' RT	870.55	474188.82	903483.86
18-17	89+43.73	18.10' RT	870.44	474188.72	903485.86

State Street NW

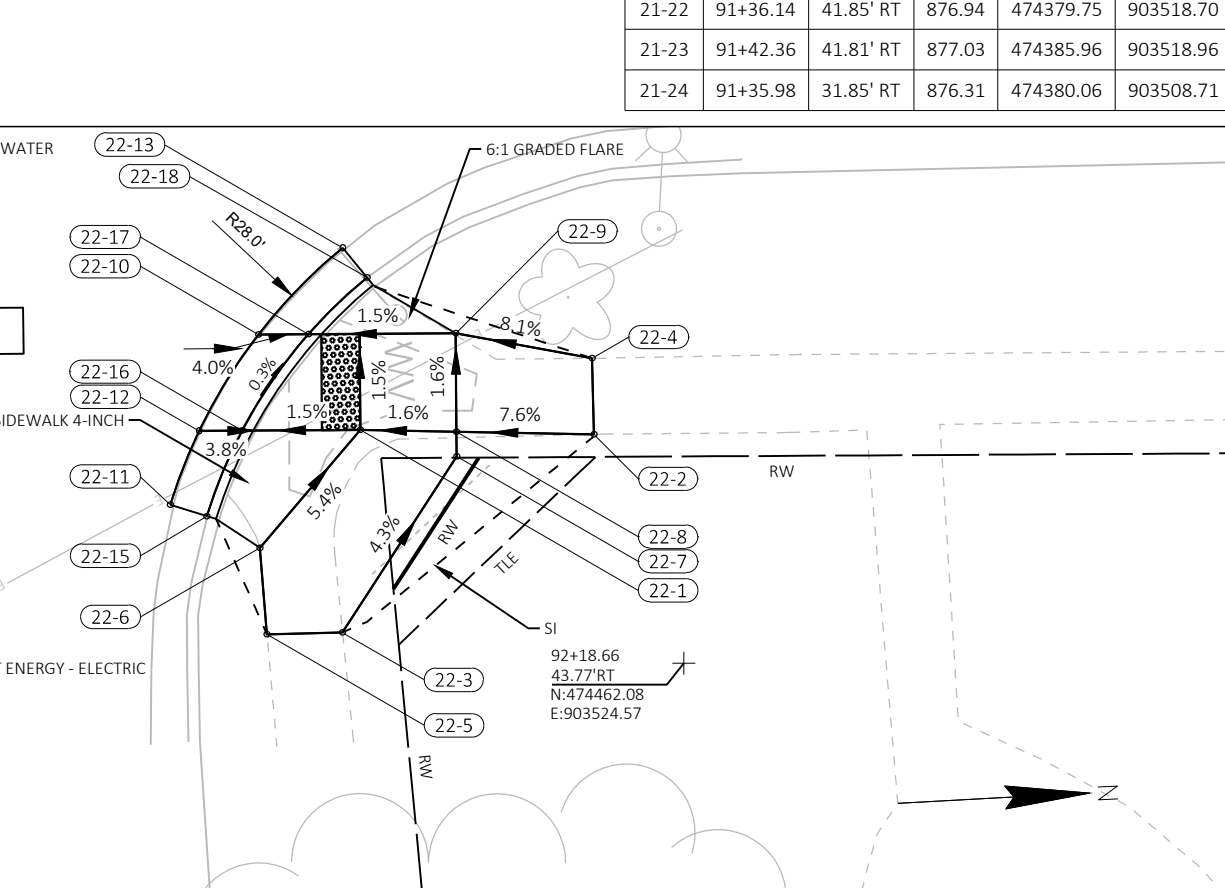
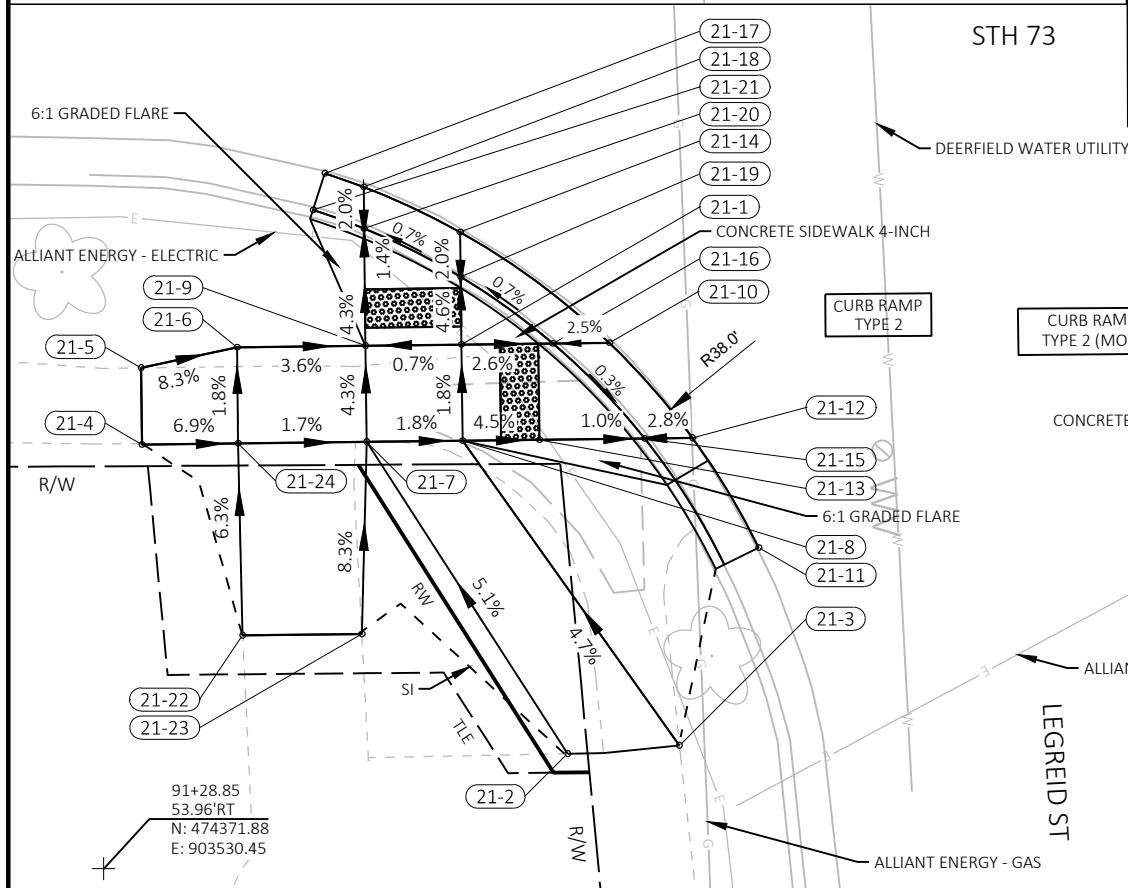
POINT	STA	OFFSET	ELEV	NORTHING	EASTING
19-1	90+06.85	27.41' LT	871.68	474253.92	903443.34
19-2	90+63.88	28.55' LT	875.44	474310.93	903444.93
19-3	90+58.87	27.38' LT	875.02	474305.88	903445.86
19-4	90+22.77	47.63' LT	873.27	474270.79	903423.90
19-5	90+63.88	32.38' LT	875.33	474311.12	903441.11
19-6	90+19.05	46.62' LT	872.99	474267.03	903424.73
19-7	90+28.02	32.40' LT	872.51	474275.30	903439.37
19-8	90+23.88	32.39' LT	872.17	474271.17	903439.18
19-9	90+09.66	27.41' LT	871.57	474256.73	903443.48
19-10	90+17.39	27.41' LT	871.65	474264.44	903443.85
19-11	90+17.39	32.40' LT	871.64	474264.69	903438.86
19-12	89+98.89	46.26' LT	870.88	474246.87	903424.13
19-13	90+17.39	34.44' LT	871.58	474264.78	903436.82
19-14	90+12.39	32.40' LT	871.56	474259.69	903438.62
19-16	90+05.28	32.41' LT	871.40	474252.59	903438.27
19-17	90+02.83	32.41' LT	871.36	474250.14	903438.15
19-18	90+02.73	36.32' LT	871.12	474250.23	903434.24
19-19	90+02.18	50.03' LT	870.99	474250.34	903420.52
19-20	89+98.26	49.67' LT	870.88	474246.40	903420.70
19-21	90+01.03	35.26' LT	871.24	474248.49	903435.22
19-22	90+11.09	23.48' LT	872.12	474257.96	903447.47
19-23	90+12.25	25.12' LT	872.01	474259.20	903445.89
19-24	90+11.05	26.91' LT	871.97	474258.09	903444.04
19-25	90+17.39	26.90' LT	872.15	474264.42	903444.35
19-26	90+58.93	26.88' LT	875.22	474305.92	903446.36
19-27	90+63.99	28.06' LT	875.43	474311.02	903445.42



LEGREID SW					
POINT	STA	OFFSET	ELEV	NORTHING	EASTING
20-1	91+42.59	32.46' LT	876.51	474389.74	903444.79
20-2	91+54.86	28.60' LT	877.10	474401.81	903449.23
20-3	91+54.83	32.42' LT	877.09	474401.97	903445.41
20-4	91+52.56	27.42' LT	876.89	474399.46	903450.30
20-5	91+34.88	32.49' LT	876.87	474382.05	903444.39
20-6	91+34.86	28.57' LT	876.76	474381.83	903448.31
20-7	91+37.56	27.50' LT	876.62	474384.48	903449.50
20-8	91+47.59	32.45' LT	876.60	474394.74	903445.04
20-9	91+47.56	27.45' LT	876.51	474394.47	903450.03
20-10	91+42.56	27.47' LT	876.42	474389.47	903449.77
20-11	91+59.47	16.89' LT	876.06	474405.86	903461.15
20-12	91+59.47	18.89' LT	876.02	474405.95	903459.15
20-13	91+47.49	16.87' LT	875.95	474393.89	903460.59
20-14	91+42.49	16.87' LT	875.90	474388.90	903460.36
20-15	91+47.50	18.87' LT	875.91	474394.00	903458.59
20-16	91+39.40	16.86' LT	875.87	474385.81	903460.21
20-17	91+42.50	18.87' LT	875.86	474389.01	903458.36
20-18	91+39.41	18.86' LT	875.81	474385.92	903458.22

LEGREID SE					
POINT	STA	OFFSET	ELEV	NORTHING	EASTING
21-1	91+47.64	26.77' RT	876.02	474391.96	903504.19
21-2	91+53.04	48.11' RT	877.17	474396.33	903525.76
21-3	91+58.87	47.71' RT	877.02	474402.17	903525.64
21-4	91+30.98	31.88' RT	876.65	474375.07	903508.50
21-5	91+30.94	27.89' RT	876.64	474375.23	903504.51
21-6	91+35.95	26.84' RT	876.22	474380.27	903503.70
21-7	91+42.67	31.81' RT	876.20	474386.75	903508.98
21-8	91+47.67	31.78' RT	876.11	474391.75	903509.19
21-9	91+42.64	26.80' RT	875.98	474386.96	903503.98
21-10	91+55.34	26.73' RT	875.97	474399.65	903504.52
21-11	91+63.06	37.44' RT	875.96	474406.85	903515.58
21-12	91+59.64	31.70' RT	875.94	474403.71	903509.69
21-13	91+51.67	31.75' RT	875.93	474395.74	903509.36
21-14	91+47.61	20.92' RT	875.90	474392.20	903498.34
21-15	91+57.15	31.72' RT	875.87	474401.22	903509.59
21-16	91+52.41	26.74' RT	875.89	474396.72	903504.39
21-17	91+40.57	17.82' RT	875.87	474385.32	903494.91
21-18	91+42.59	18.54' RT	875.86	474387.31	903495.73

LEGREID NE					
POINT	STA	OFFSET	ELEV	NORTHING	EASTING
22-1	92+01.88	31.52' RT	875.95	474445.91	903511.53
22-2	92+14.04	31.82' RT	876.58	474458.04	903512.42
22-3	92+00.89	42.07' RT	876.56	474444.41	903522.02
22-4	92+13.96	27.86' RT	876.53	474458.15	903508.46
22-5	91+96.96	42.14' RT	876.41	474440.48	903521.91
22-6	91+96.61	37.63' RT	876.39	474440.35	903517.38
22-7	92+06.88	32.94' RT	876.09	474450.83	903513.18
22-8	92+06.88	31.65' RT	876.04	474450.89	903511.90
22-9	92+06.88	26.52' RT	875.95	474451.14	903506.77
22-10	91+96.60	26.52' RT	875.95	474440.87	903506.29
22-11	91+91.95	35.37' RT	875.96	474435.80	903514.90
22-12	91+93.48	31.53' RT	875.95	474437.51	903511.14
22-13	92+01.01	22.03' RT	875.90	474445.49	903502.01
22-15	91+93.86	35.97' RT	875.89	474437.68	903515.59
22-16	91+95.72	31.53' RT	875.86	474439.76	903511.24
22-17	91+99.20	26.52' RT	875.84	474443.47	903506.41
22-18	92+02.27	23.59' RT	875.82	474446.67	903503.62



PROJECT NO: 3070-04-60

HWY: STH 73

COUNTY: DANE

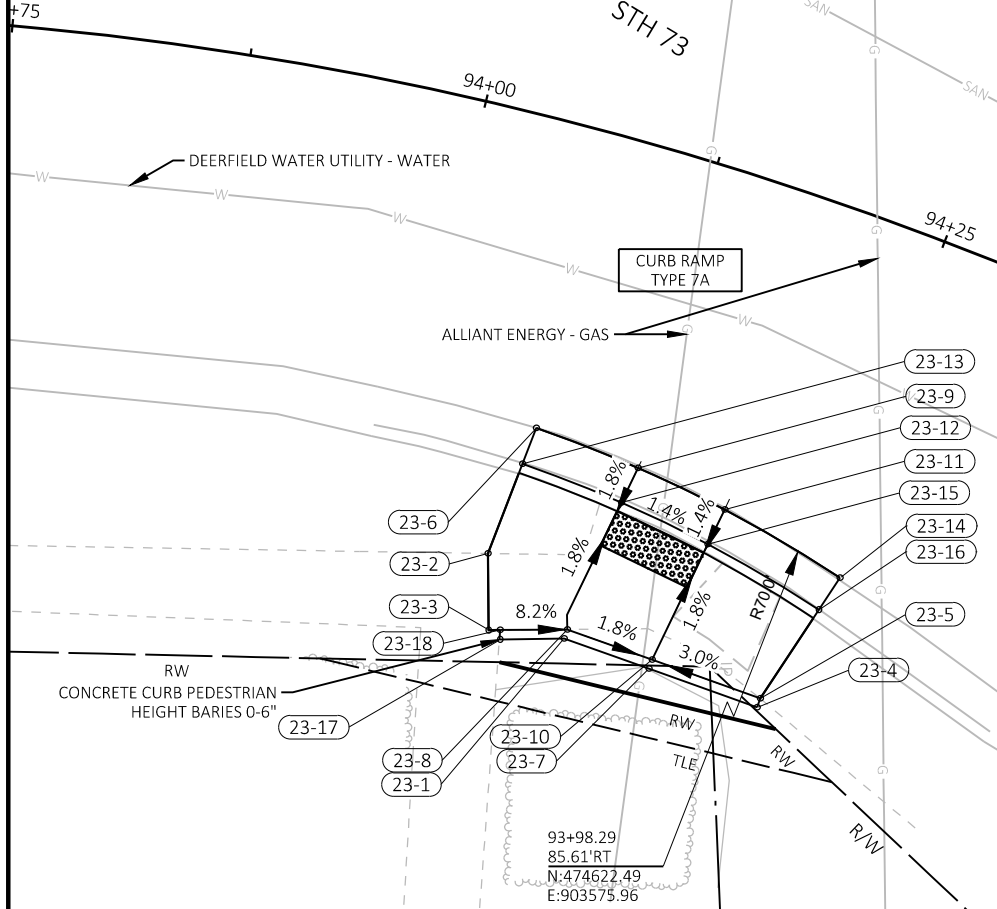
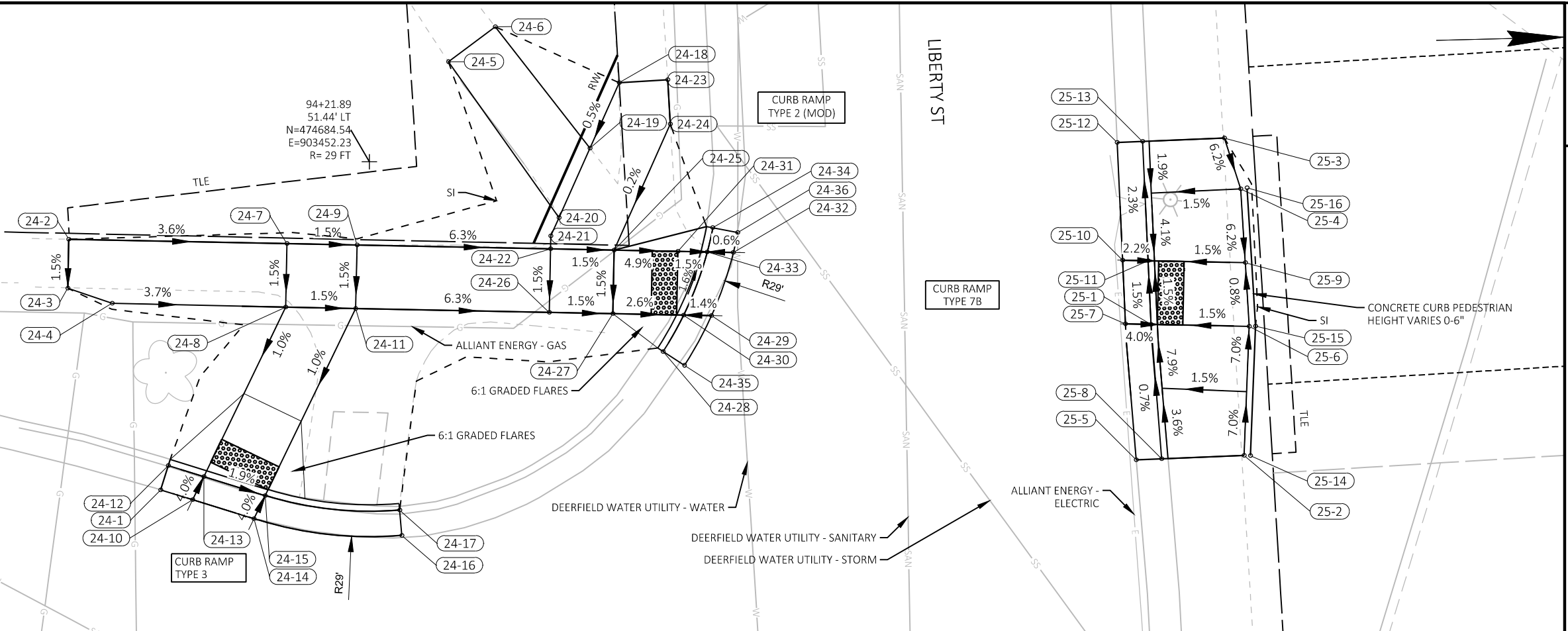
PAVING DETAILS - CURB RAMPS

SHEET

2

2

LIBERTY SE					
POINT	STA	OFFSET	ELEV	NORTHING	EASTING
23-1	94+12.01	26.00' RT	872.74	474648.19	903521.39
23-2	94+06.20	22.82' RT	872.82	474644.35	903516.86
23-3	94+07.46	26.67' RT	872.80	474644.28	903520.85
23-4	94+24.49	26.15' RT	872.59	474658.14	903525.25
23-5	94+24.48	25.65' RT	872.60	474658.32	903524.78
23-6	94+07.01	15.87' RT	872.46	474647.03	903510.40
23-7	94+17.49	26.16' RT	872.71	474652.56	903523.08
23-8	94+12.04	25.50' RT	872.51	474648.37	903520.92
23-9	94+13.24	16.37' RT	872.41	474652.28	903512.61
23-10	94+17.50	25.66' RT	872.42	474652.74	903522.62
23-11	94+18.70	17.03' RT	872.33	474656.73	903514.90
23-12	94+12.91	18.35' RT	872.37	474651.36	903514.39
23-13	94+06.78	17.86' RT	872.31	474646.26	903512.25
23-14	94+26.32	18.30' RT	872.25	474662.64	903518.61
23-15	94+18.44	19.02' RT	872.30	474655.81	903516.68
23-16	94+25.85	20.25' RT	872.14	474661.49	903520.25
23-17	94+08.28	26.99' RT	872.79	474644.86	903521.36
23-18	94+08.12	26.51' RT	872.79	474644.87	903520.86



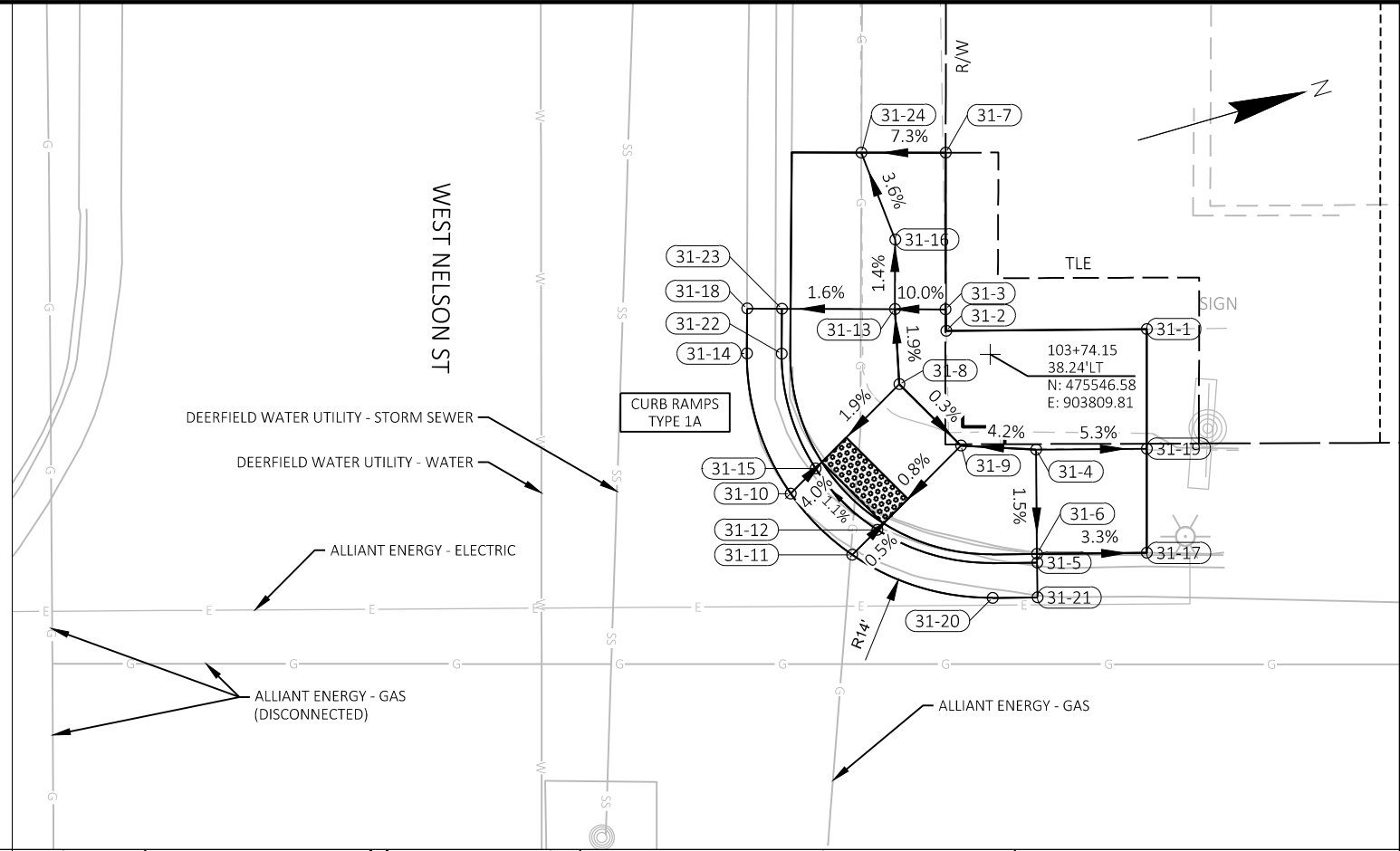
LIBERTY SW					
POINT	STA	OFFSET	ELEV	NORTHING	EASTING
24-1	94+16.01	21.70' LT	871.95	474667.46	903477.59
24-2	94+05.29	38.56' LT	872.66	474660.74	903457.69
24-3	94+06.05	34.83' LT	872.60	474660.56	903461.53
24-4	94+09.13	34.62' LT	872.46	474664.02	903462.81
24-5	94+24.25	61.00' LT	872.25	474690.96	903444.52
24-6	94+26.07	64.89' LT	872.10	474694.72	903441.87
24-7	94+18.76	43.22' LT	872.03	474677.89	903458.49
24-8	94+20.03	38.47' LT	871.96	474677.66	903463.48
24-9	94+22.96	44.97' LT	871.95	474683.40	903458.74
24-10	94+18.35	21.76' LT	871.89	474669.94	903478.43
24-11	94+24.30	40.25' LT	871.87	474683.16	903463.74
24-12	94+15.98	23.70' LT	871.89	474668.10	903475.69
24-13	94+18.57	23.76' LT	871.81	474670.87	903476.63
24-14	94+22.86	21.97' LT	871.79	474674.73	903480.05
24-15	94+23.03	23.98' LT	871.71	474675.66	903478.25
24-16	94+32.83	25.08' LT	871.56	474686.32	903481.67
24-17	94+32.00	26.84' LT	871.52	474686.20	903479.68
24-18	94+33.93	64.56' LT	871.00	474704.34	903446.52

LIBERTY SW					
POINT	STA	OFFSET	ELEV	NORTHING	EASTING
24-19	94+33.91	58.93' LT	871.03	474701.91	903451.60
24-20	94+33.89	52.94' LT	871.00	474699.33	903457.01
24-21	94+33.89	51.36' LT	871.00	474698.65	903458.43
24-22	94+34.19	50.43' LT	870.99	474698.60	903459.44
24-23	94+36.41	66.34' LT	870.95	474708.16	903446.39
24-24	94+37.62	63.26' LT	870.94	474708.28	903449.88
24-25	94+37.76	52.41' LT	870.92	474703.60	903459.67
24-26	94+35.74	45.83' LT	870.92	474698.37	903464.44
24-27	94+39.37	47.85' LT	870.84	474703.37	903464.67
24-28	94+43.21	46.92' LT	870.78	474707.24	903467.75
24-29	94+44.87	51.16' LT	870.75	474711.12	903465.03
24-30	94+43.33	50.21' LT	870.72	474708.93	903464.93
24-31	94+41.28	54.49' LT	870.67	474708.61	903459.91
24-32	94+44.29	56.37' LT	870.65	474712.97	903460.11
24-33	94+42.88	55.47' LT	870.63	474710.91	903460.01
24-34	94+42.53	57.37' LT	870.60	474711.40	903458.12
24-35	94+44.79	46.72' LT	870.84	474708.89	903468.88
24-36	94+43.98	57.92' LT	870.62	474713.35	903458.55

LIBERTY NW					
POINT	STA	OFFSET	ELEV	NORTHING	EASTING
25-1	94+67.42	68.37' LT	870.27	474745.66	903466.62
25-2	94+76.17	64.31' LT	871.09	474752.71	903477.12
25-3	94+65.00	83.74' LT	870.88	474751.82	903452.13
25-4	94+67.27	81.15' LT	870.62	474753.00	903456.16
25-5	94+71.24	58.98' LT	870.39	474744.22	903477.25
25-6	94+72.01	72.70' LT	870.38	474753.38	903466.98
25-7	94+66.20	67.27' LT	870.35	474743.66	903466.53
25-8	94+72.42	60.22' LT	870.34	474746.22	903477.22
25-9	94+69.78	76.59' LT	870.31	474753.20	903461.97
25-10	94+64.05	71.30' LT	870.24	474743.56	903461.52
25-11	94+65.26	72.38' LT	870.19	474745.56	903461.61
25-12	94+60.26	78.85' LT	870.03	474743.38	903452.26
25-13	94+61.40	79.99' LT	869.98	474745.38	903452.23
25-14	94+76.47	64.60' LT	871.08	474753.21	903477.15
25-15	94+72.30	73.00' LT	870.92	474753.88	903466.99
25-16	94+67.51	81.50' LT	870.80	474753.49	903456.08

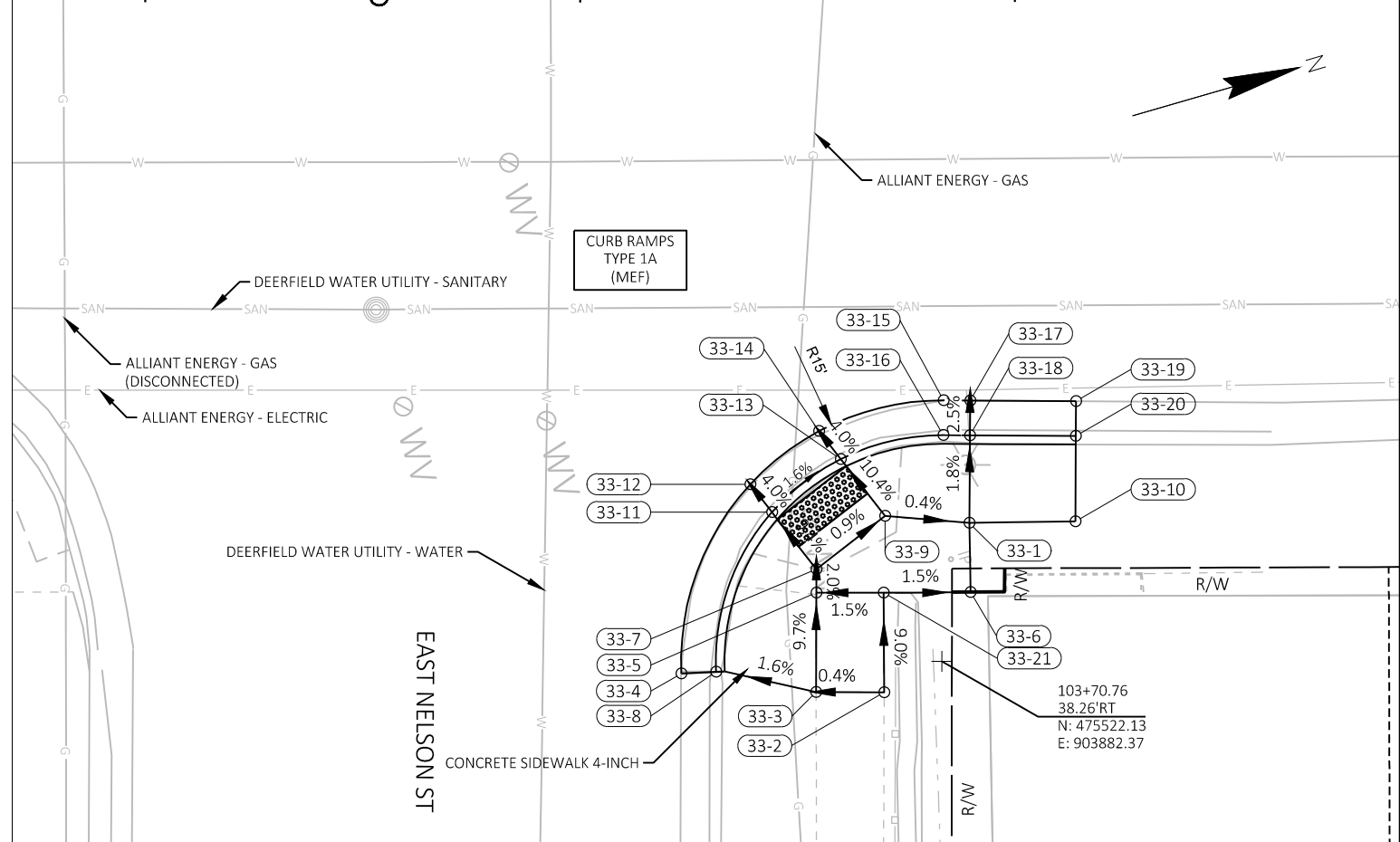
NELSON NW					
POINT	STA	OFFSET	ELEV	NORTHING	EASTING
31-1	103+83.15	39.69' LT	864.56	475555.64	903810.91
31-2	103+71.61	39.60' LT	864.82	475544.52	903807.80
31-3	103+71.57	40.84' LT	864.79	475544.83	903806.60
31-4	103+76.77	32.76' LT	864.74	475547.59	903815.80
31-5	103+76.82	26.26' LT	864.65	475545.84	903822.06
31-6	103+76.82	26.76' LT	864.65	475545.97	903821.58
31-7	103+71.60	49.84' LT	864.60	475547.35	903797.95
31-8	103+68.92	36.53' LT	864.58	475541.09	903810.00
31-9	103+72.46	33.00' LT	864.56	475543.51	903814.37
31-10	103+62.65	30.25' LT	864.53	475533.32	903814.30
31-11	103+66.19	26.72' LT	864.52	475535.75	903818.67
31-12	103+67.63	28.16' LT	864.51	475537.53	903817.68
31-13	103+68.66	40.85' LT	864.50	475542.04	903805.78
31-14	103+60.15	38.31' LT	864.48	475533.15	903805.86
31-15	103+64.09	31.69' LT	864.45	475535.11	903813.31

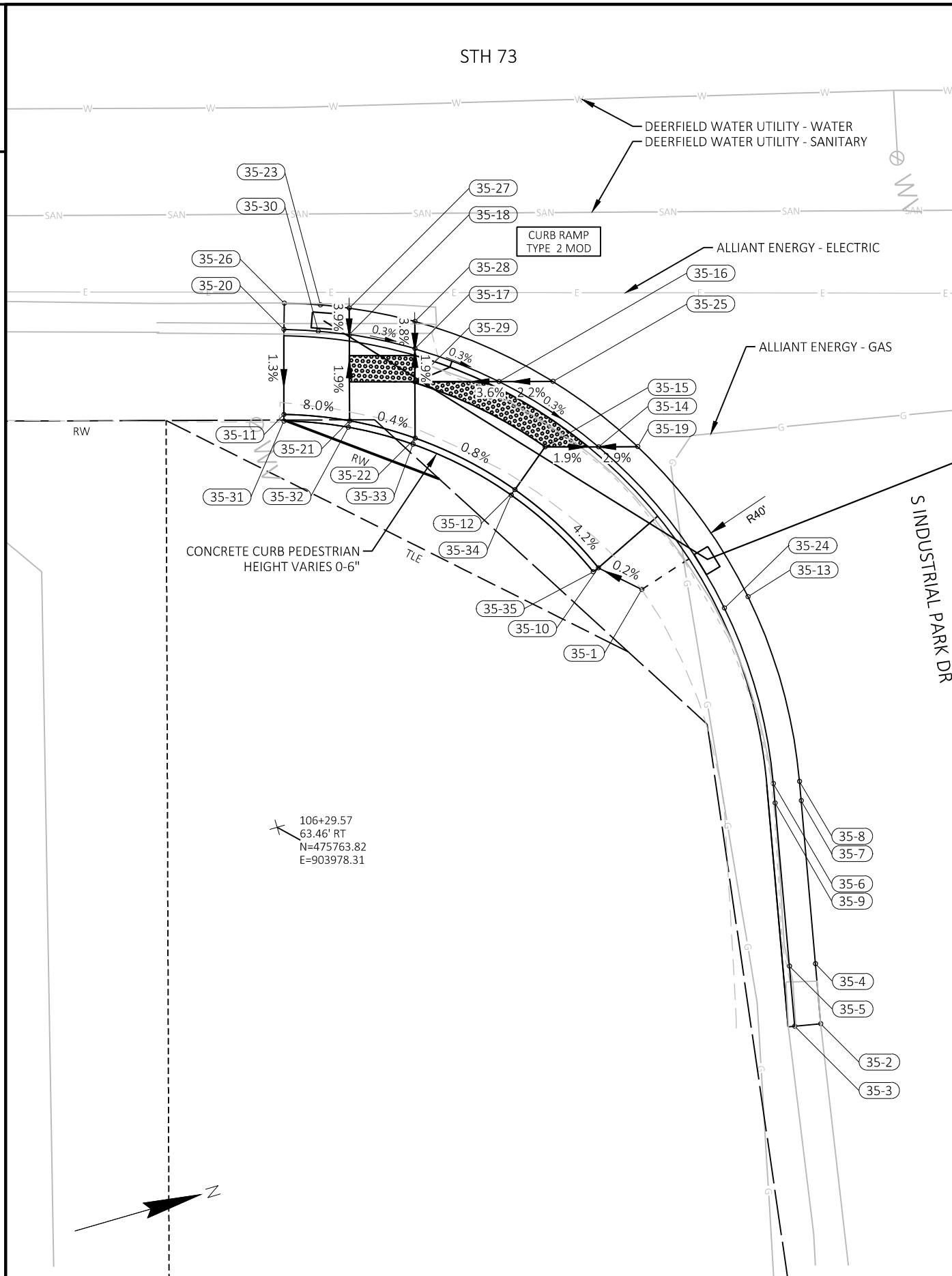
NELSON NW					
POINT	STA	OFFSET	ELEV	NORTHING	EASTING
31-16	103+68.68	44.85' LT	864.45	475543.17	903801.94
31-17	103+83.13	26.81' LT	864.45	475552.05	903823.28
31-18	103+60.16	40.89' LT	864.41	475533.88	903803.38
31-19	103+83.14	32.80' LT	864.40	475553.72	903817.52
31-20	103+74.25	24.24' LT	864.36	475542.81	903823.29
31-21	103+76.84	24.26' LT	864.34	475545.30	903823.99
31-22	103+62.15	38.30' LT	864.33	475535.07	903806.42
31-23	103+62.16	40.88' LT	864.28	475535.80	903803.94
31-24	103+66.75	49.86' LT	864.25	475542.70	903796.59



NELSON NE (MEF)					
POINT	STA	OFFSET	ELEV	NORTHING	EASTING
33-1	103+72.34	30.30' RT	865.85	475525.86	903875.16
33-2	103+67.43	40.04' RT	866.51	475518.44	903883.16
33-3	103+63.52	40.02' RT	866.49	475514.68	903882.06
33-4	103+55.78	38.94' RT	866.03	475507.55	903878.87
33-5	103+63.54	34.30' RT	865.94	475516.29	903876.56
33-6	103+72.41	34.28' RT	865.92	475524.82	903879.01
33-7	103+63.54	32.97' RT	865.91	475516.66	903875.28
33-8	103+57.77	38.85' RT	865.91	475509.49	903879.34
33-9	103+67.49	29.91' RT	865.87	475521.31	903873.44
33-10	103+78.45	30.24' RT	865.64	475531.75	903876.79
33-11	103+60.99	29.68' RT	865.52	475515.13	903871.42
33-12	103+59.75	28.07' RT	865.44	475514.38	903869.53
33-13	103+64.95	26.63' RT	865.44	475519.78	903869.58
33-14	103+63.71	25.02' RT	865.36	475519.03	903867.69
33-15	103+70.87	23.26' RT	865.24	475526.40	903867.99

NELSON NE (MEF)					
POINT	STA	OFFSET	ELEV	NORTHING	EASTING
33-16	103+70.86	25.26' RT	865.22	475525.83	903869.91
33-17	103+72.40	23.27' RT	865.22	475527.86	903868.42
33-18	103+72.38	25.27' RT	865.27	475527.29	903870.34
33-19	103+78.48	23.32' RT	865.11	475533.69	903870.16
33-20	103+78.47	25.32' RT	865.08	475533.13	903872.07
33-21	103+67.41	34.31' RT	866.00	475520.01	903877.64

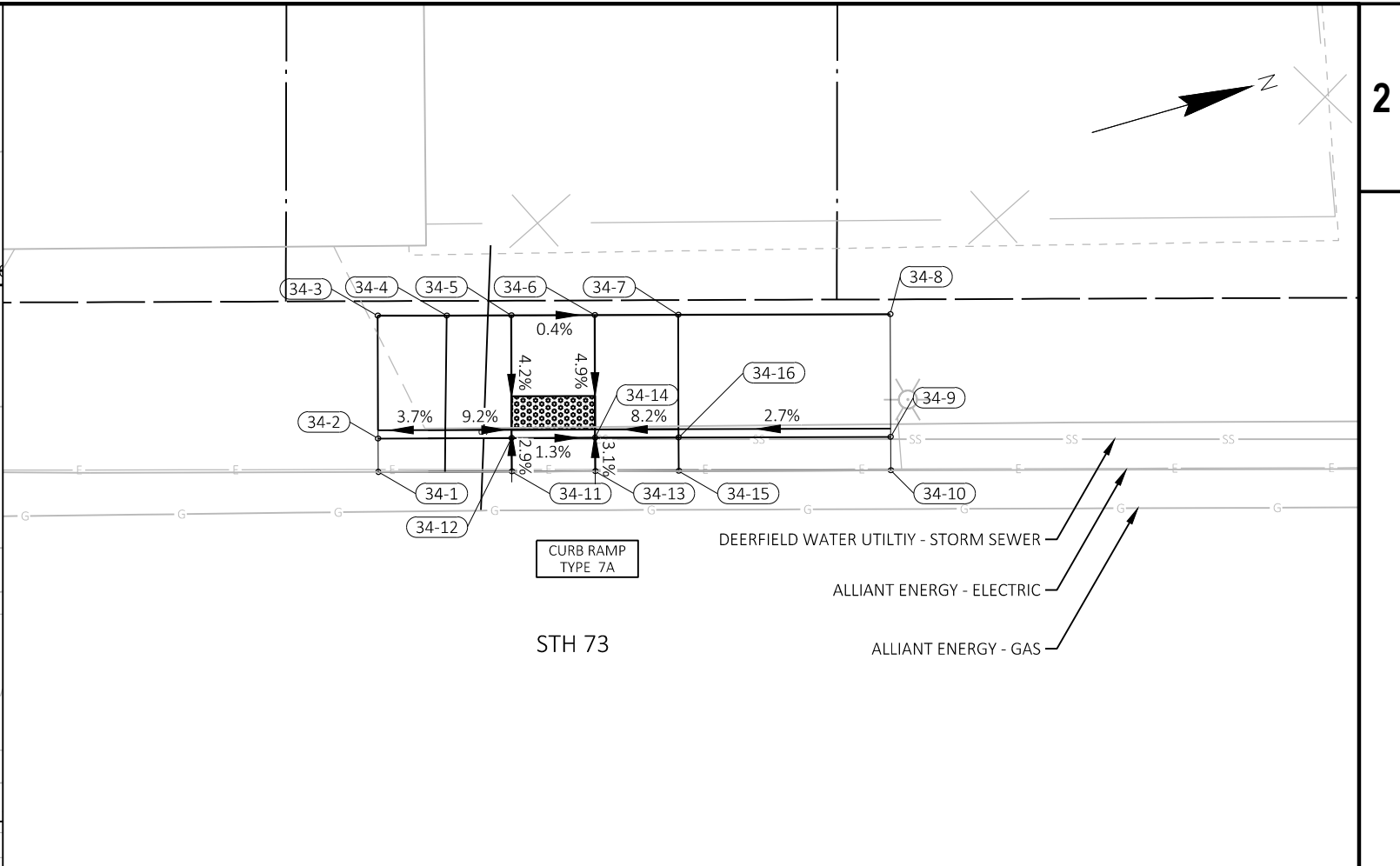
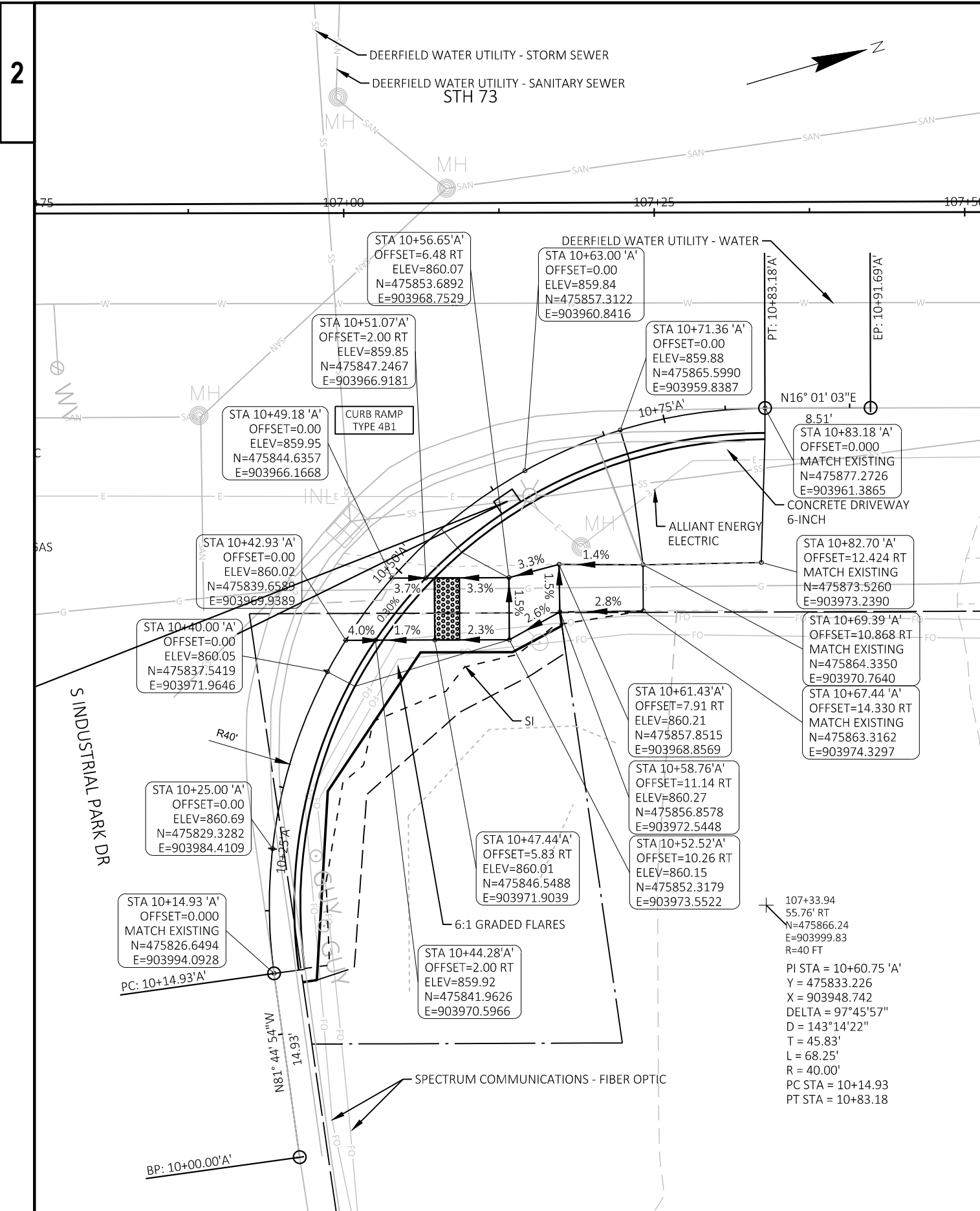




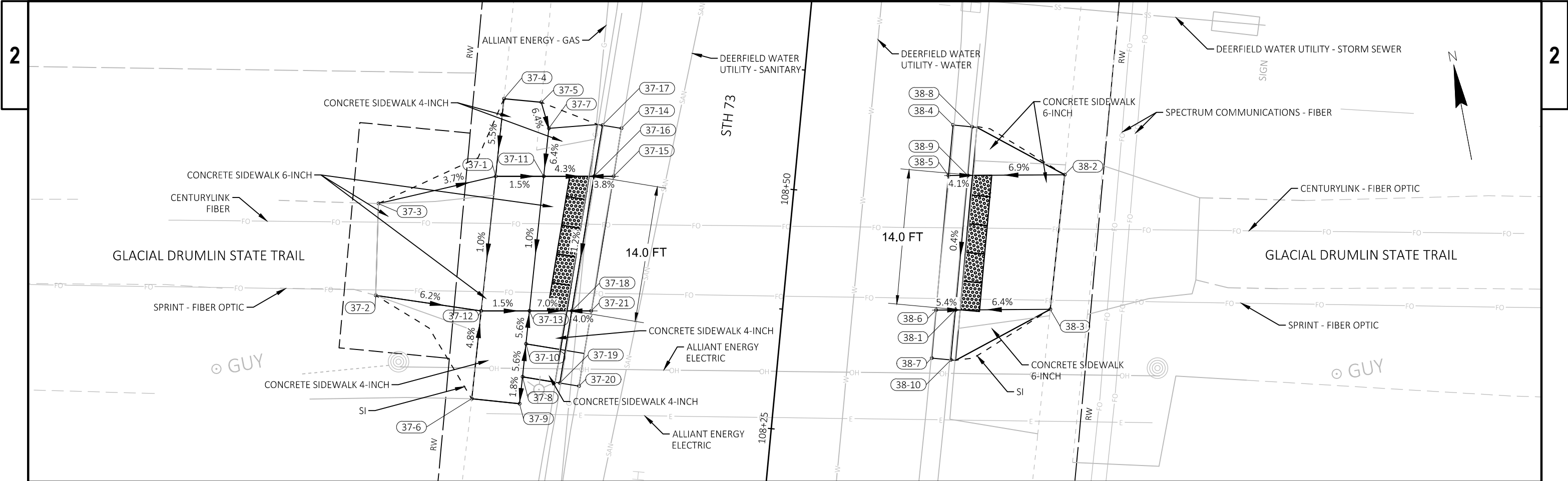
RADIAL DETECTABLE WARNING FIELD

BACK OF CURB RADIUS = 37.5 FT
 LANDING LENGTH 'XR' = 13.22 FT
 RADIAL WARNING FIELD AREA = 20.54 SQ FT
 RADIAL LONG CHORD = 9.38 FT

South Industrial Park Drive - SE					
POINT	STA	OFFSET	ELEV	NORTHING	EASTING
35-1	106+57.41	45.35' RT	861.24	475795.59	903968.63
35-2	106+71.02	78.52' RT	861.76	475799.48	904004.27
35-3	106+69.03	78.69' RT	861.72	475797.51	904003.88
35-4	106+70.63	73.94' RT	861.58	475800.37	903999.75
35-5	106+68.64	74.11' RT	861.50	475798.40	903999.37
35-6	106+67.43	60.18' RT	861.25	475801.10	903985.65
35-7	106+69.55	61.48' RT	861.38	475802.78	903987.49
35-8	106+69.42	60.01' RT	861.34	475803.07	903986.03
35-9	106+67.56	61.66' RT	861.30	475800.82	903987.10
35-10	106+54.10	43.69' RT	861.21	475792.86	903966.11
35-11	106+30.13	31.96' RT	861.16	475773.09	903948.20
35-12	106+47.74	37.73' RT	860.85	475788.41	903958.62
35-13	106+65.51	45.90' RT	860.87	475803.22	903971.40
35-14	106+54.13	34.46' RT	860.70	475795.45	903957.25
35-15	106+50.05	34.46' RT	860.77	475791.54	903956.12
35-16	106+46.54	29.46' RT	860.67	475789.55	903950.35
35-17	106+40.12	26.95' RT	860.65	475784.08	903946.15
35-18	106+35.12	25.86' RT	860.63	475779.58	903943.72
35-19	106+57.12	34.45' RT	860.79	475798.33	903958.07
35-20	106+30.13	25.46' RT	860.72	475774.89	903941.96
35-21	106+35.13	32.45' RT	860.76	475777.76	903950.06
35-22	106+40.13	33.78' RT	860.78	475782.19	903952.72
35-23	106+32.92	23.60' RT	860.71	475778.09	903940.94
35-24	106+63.71	46.78' RT	860.75	475801.25	903971.75
35-25	106+50.65	29.46' RT	860.76	475793.49	903951.48
35-26	106+30.16	23.46' RT	860.70	475775.47	903940.04
35-27	106+35.12	23.84' RT	860.71	475780.13	903941.78
35-28	106+40.12	24.87' RT	860.73	475784.65	903944.16
35-29	106+40.13	29.47' RT	860.70	475783.38	903948.58
35-30	106+32.75	25.59' RT	860.63	475777.37	903942.81
35-31	106+30.12	32.46' RT	861.18	475772.94	903948.68
35-32	106+35.04	32.94' RT	861.13	475777.54	903950.50
35-33	106+39.96	34.25' RT	861.11	475781.90	903953.12
35-34	106+47.46	38.14' RT	861.09	475788.02	903958.93
35-35	106+53.71	44.00' RT	861.22	475792.40	903966.30



SOUTH INDUSTRIAL PARK DRIVE - SW					
POINT	STA	OFFSET	ELEV	NORTHING	EASTING
34-1	106+27.06	23.39' LT	860.62	475785.48	903894.17
34-2	106+27.06	25.39' LT	860.57	475786.03	903892.25
34-3	106+27.05	32.74' LT	860.79	475788.06	903885.18
34-4	106+31.18	32.74' LT	860.78	475792.03	903886.32
34-5	106+35.05	32.75' LT	860.77	475795.75	903887.38
34-6	106+40.05	32.76' LT	860.75	475800.55	903888.76
34-7	106+45.04	32.77' LT	860.72	475805.36	903890.14
34-8	106+57.75	32.79' LT	860.72	475817.57	903893.64
34-9	106+57.76	25.44' LT	860.16	475815.54	903900.71
34-10	106+57.77	23.44' LT	860.24	475814.99	903902.63
34-11	106+35.06	23.40' LT	860.52	475793.17	903896.37
34-12	106+35.06	25.40' LT	860.50	475793.72	903894.45
34-13	106+40.06	23.41' LT	860.46	475797.98	903897.75
34-14	106+40.06	25.41' LT	860.44	475798.53	903895.83
34-15	106+45.06	23.42' LT	860.40	475802.78	903899.13
34-16	106+45.06	25.42' LT	860.36	475803.33	903897.21



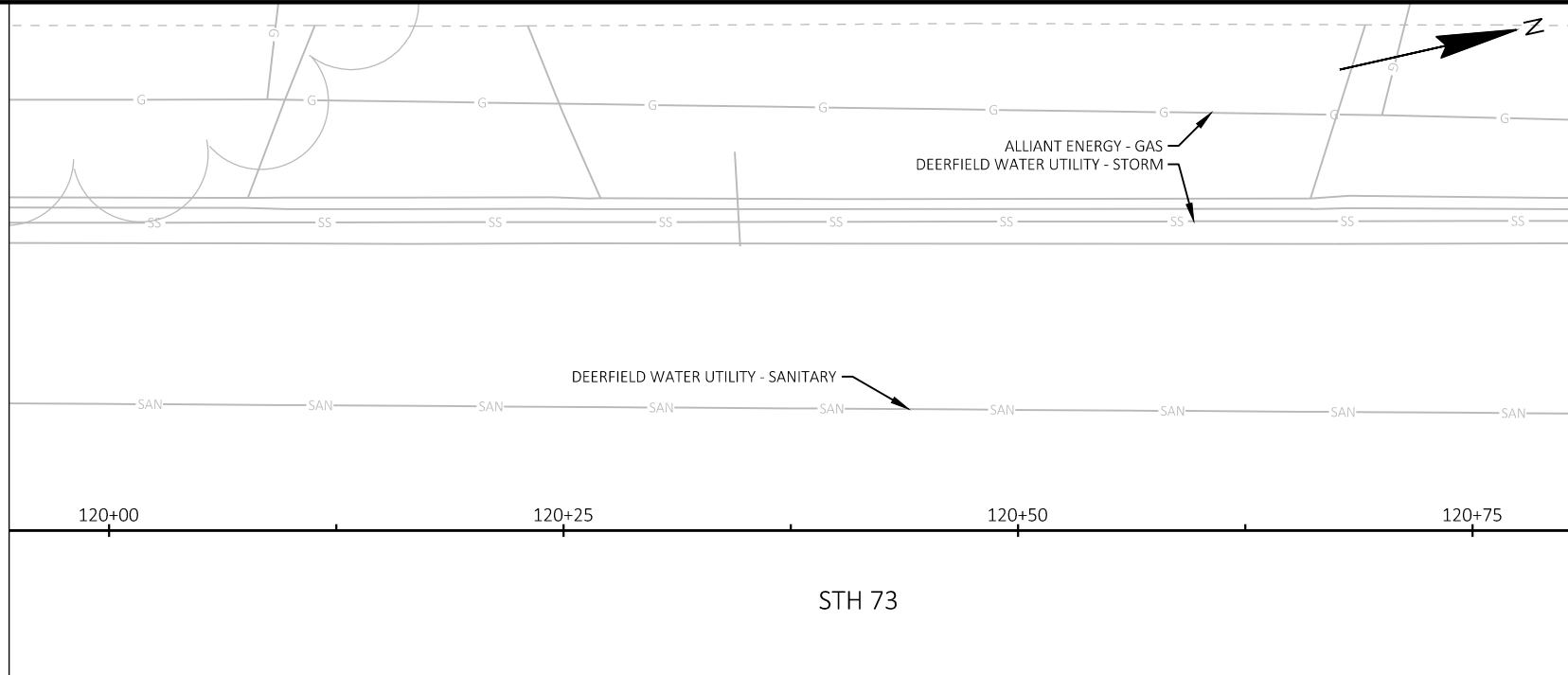
Glacial Drumlin - West					
POINT	STA	OFFSET	ELEV	NORTHING	EASTING
37-1	108+48.45	31.02' LT	860.41	476000.31	903948.19
37-2	108+34.94	42.29' LT	860.95	475990.45	903933.62
37-3	108+44.49	42.91' LT	860.88	475999.80	903935.67
37-4	108+56.59	30.92' LT	860.86	476008.10	903950.54
37-5	108+56.58	27.03' LT	860.84	476007.01	903954.28
37-6	108+25.17	31.33' LT	860.71	475978.03	903941.44
37-7	108+53.95	25.99' LT	860.66	476004.20	903954.55
37-8	108+27.93	26.28' LT	860.58	475979.28	903947.05
37-9	108+25.13	26.33' LT	860.53	475976.60	903946.23
37-10	108+31.39	26.25' LT	860.38	475982.60	903948.05
37-11	108+48.92	26.05' LT	860.34	475999.39	903953.10

Glacial Drumlin - West					
POINT	STA	OFFSET	ELEV	NORTHING	EASTING
37-12	108+34.37	31.22' LT	860.27	475986.84	903944.10
37-13	108+34.84	26.21' LT	860.19	475985.91	903949.04
37-14	108+54.66	18.47' LT	860.19	476002.80	903961.97
37-15	108+49.60	18.82' LT	860.19	475998.04	903960.23
37-16	108+49.41	20.84' LT	860.11	475998.41	903958.24
37-17	108+54.80	20.47' LT	860.11	476003.48	903960.09
37-18	108+35.26	21.82' LT	859.88	475985.09	903953.37
37-19	108+27.66	22.35' LT	859.87	475977.93	903950.76
37-20	108+27.52	20.35' LT	859.83	475977.24	903952.64
37-21	108+35.45	19.82' LT	859.96	475984.71	903955.35

Glacial Drumlin - East					
POINT	STA	OFFSET	ELEV	NORTHING	EASTING
38-1	108+39.10	17.91' RT	860.10	475977.77	903992.62
38-2	108+54.19	27.91' RT	860.85	475989.49	904006.40
38-3	108+40.10	27.73' RT	860.73	475976.00	904002.33
38-4	108+58.25	15.86' RT	860.26	475996.73	903995.95
38-5	108+52.97	15.88' RT	860.24	475991.65	903994.50
38-6	108+38.90	15.91' RT	860.21	475978.13	903990.64
38-7	108+33.88	15.93' RT	860.18	475973.30	903989.26
38-8	108+58.25	17.86' RT	860.17	475996.18	903997.87
38-9	108+53.17	17.87' RT	860.16	475991.30	903996.48
38-10	108+33.87	17.93' RT	860.08	475972.74	903991.18

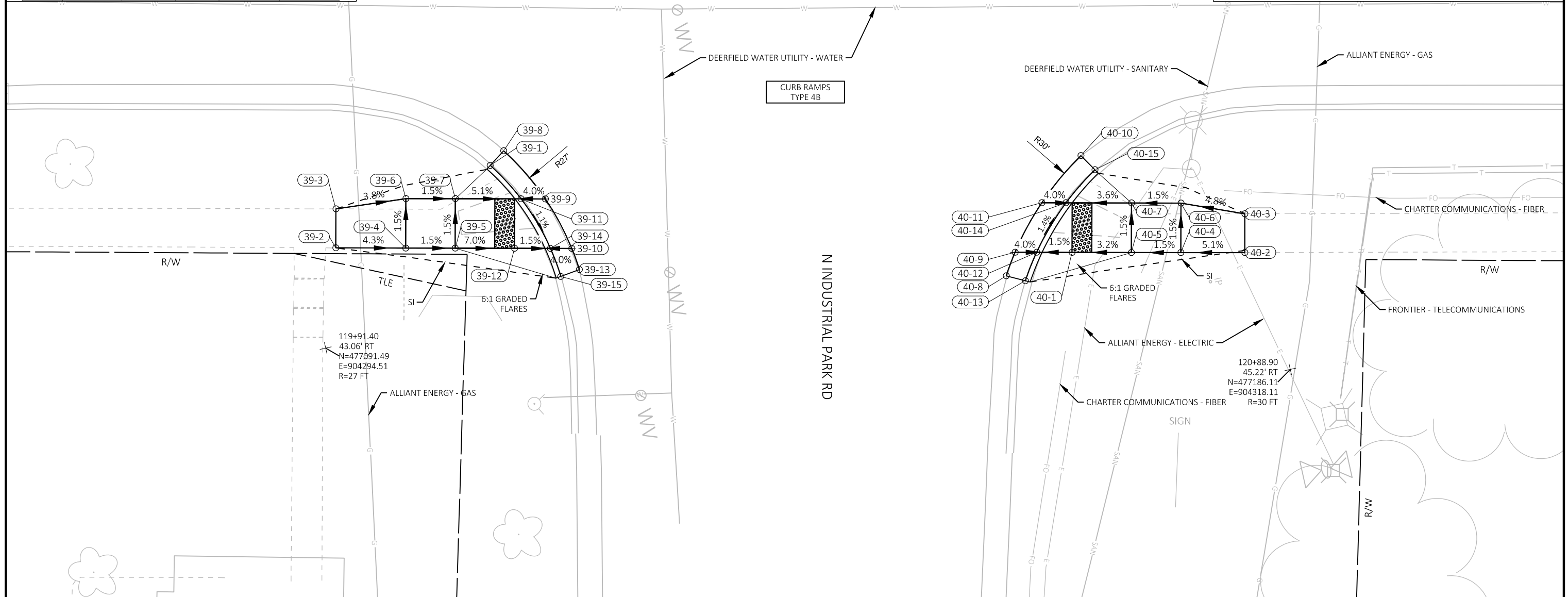
N INDUSTRIAL PARK RD - NE

POINT	STA	OFFSET	ELEV	NORTHING	EASTING
40-1	120+66.84	33.39' RT	867.97	477167.20	904301.71
40-2	120+84.31	33.41' RT	868.57	477184.24	904305.58
40-3	120+84.30	29.50' RT	868.48	477185.09	904301.77
40-4	120+77.84	33.42' RT	868.24	477177.92	904304.17
40-5	120+72.84	33.41' RT	868.17	477173.05	904303.05
40-6	120+77.85	28.42' RT	868.17	477179.04	904299.29
40-7	120+72.85	28.41' RT	868.09	477174.17	904298.18
40-8	120+60.18	35.77' RT	868.03	477160.18	904302.56
40-9	120+61.08	33.38' RT	868.01	477161.59	904300.43
40-10	120+67.74	23.62' RT	867.95	477170.24	904292.38
40-11	120+63.79	28.38' RT	867.93	477165.33	904296.16
40-12	120+63.27	33.38' RT	867.92	477163.72	904300.92
40-13	120+62.13	36.26' RT	867.84	477161.97	904303.47
40-14	120+66.23	28.39' RT	867.83	477167.71	904296.70
40-15	120+69.15	25.04' RT	867.80	477171.29	904294.08



N INDUSTRIAL PARK ROAD - SE

POINT	STA	OFFSET	ELEV	NORTHING	EASTING
39-1	120+08.03	24.61' RT	867.51	477111.77	904280.18
39-2	119+92.42	32.94' RT	868.21	477094.72	904284.86
39-3	119+92.43	29.00' RT	868.10	477095.60	904281.02
39-4	119+99.49	32.96' RT	867.91	477101.61	904286.44
39-5	120+04.49	32.97' RT	867.83	477106.48	904287.55
39-6	119+99.51	27.96' RT	867.83	477102.72	904281.56
39-7	120+04.51	27.97' RT	867.76	477107.60	904282.68
39-8	120+09.38	23.14' RT	867.60	477113.42	904279.05
39-9	120+13.61	27.99' RT	867.52	477116.47	904284.71
39-10	120+16.28	33.00' RT	867.44	477117.97	904290.18
39-11	120+11.14	27.99' RT	867.42	477114.06	904284.16
39-12	120+10.49	32.99' RT	867.41	477112.33	904288.89
39-13	120+17.04	35.13' RT	867.40	477118.24	904292.42
39-14	120+14.11	33.00' RT	867.36	477115.85	904289.70
39-15	120+15.16	35.82' RT	867.34	477116.26	904292.69



PROJECT NO: 3070-04-60

HWY: STH 73

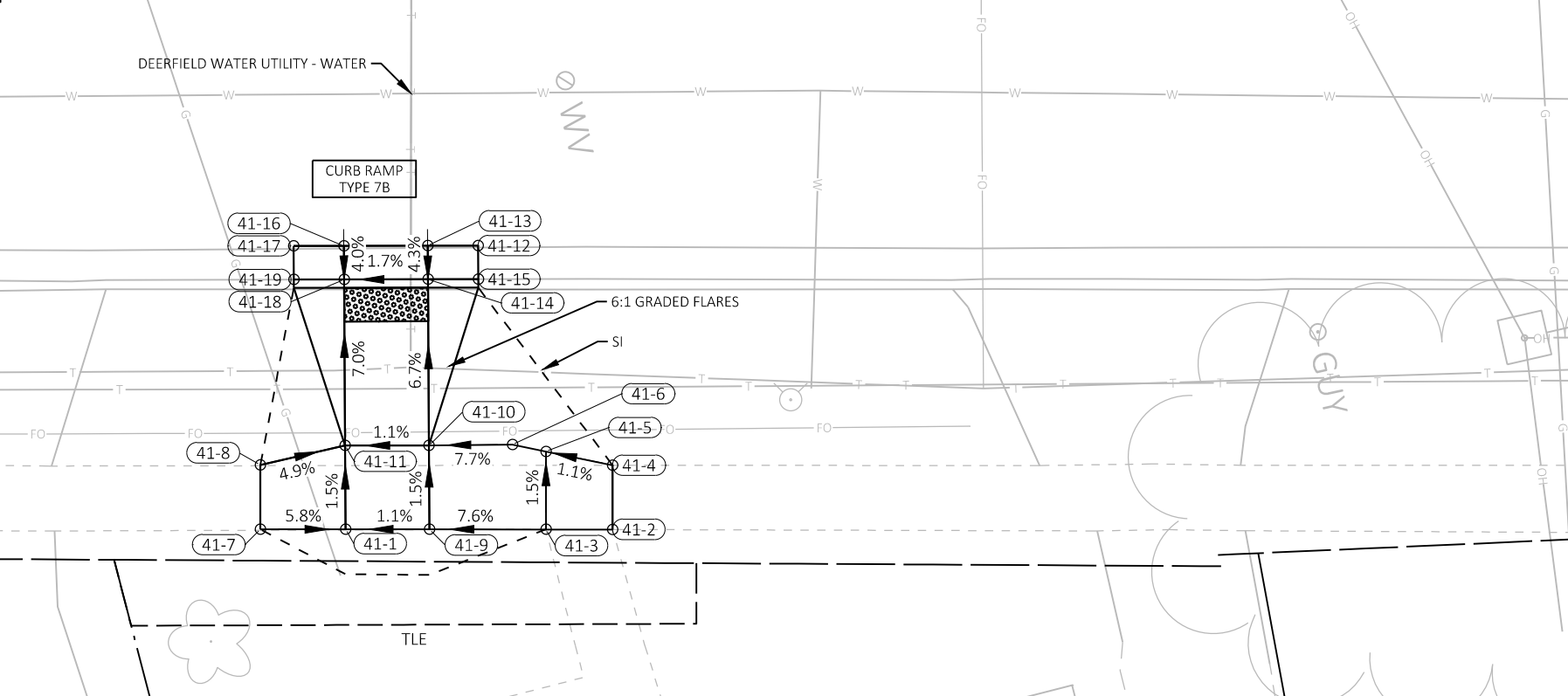
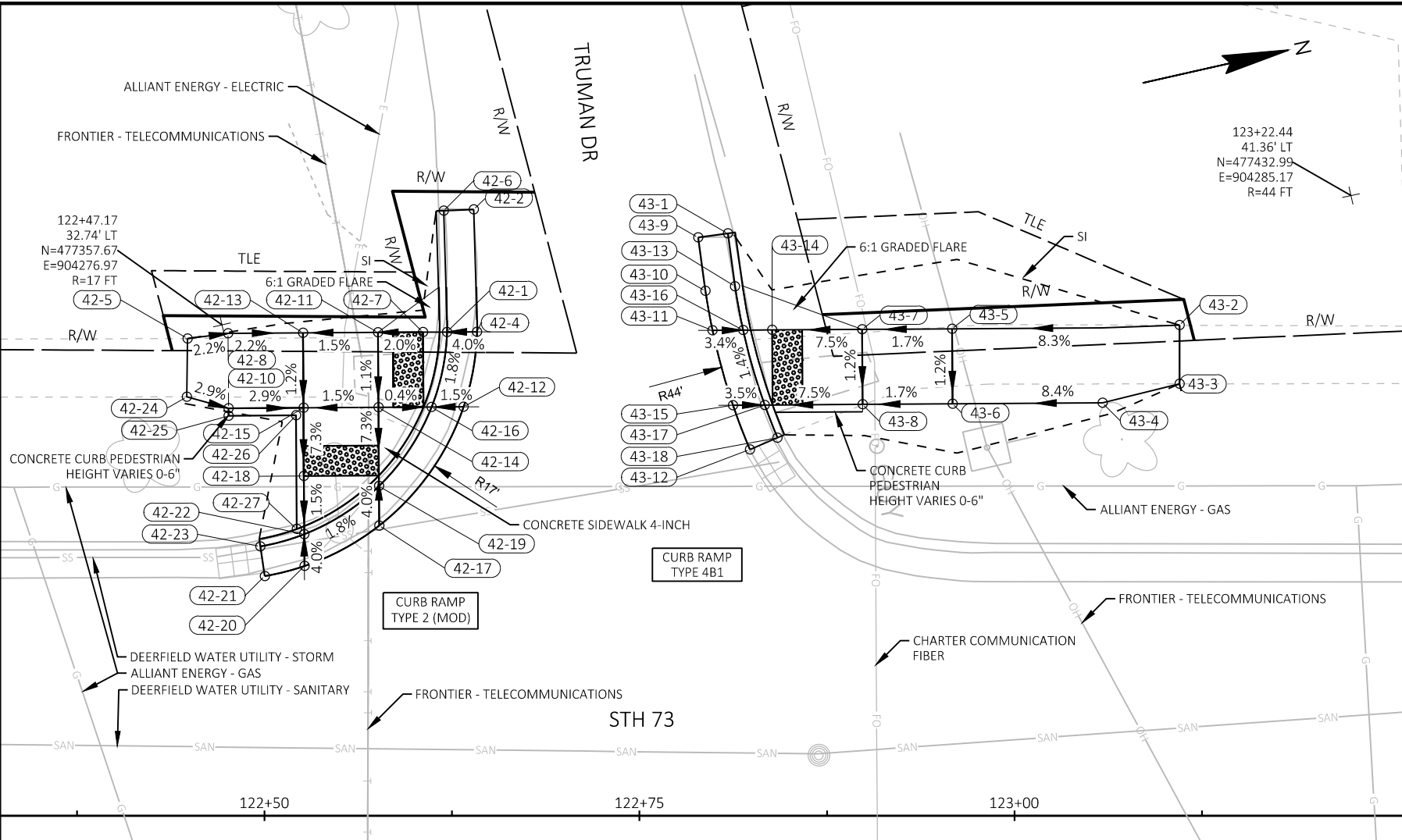
COUNTY: DANE

PAVING DETAILS - CURB RAMPS

SHEET

2

TRUMAN DR - SW					
POINT	STA	OFFSET	ELEV	NORTHING	EASTING
42-1	122+62.16	32.26' LT	870.68	477372.19	904280.75
42-2	122+63.94	40.43' LT	870.82	477375.73	904273.17
42-4	122+64.16	32.28' LT	870.76	477374.15	904281.18
42-5	122+44.86	31.83' LT	870.74	477355.22	904277.36
42-6	122+61.94	40.36' LT	870.71	477373.76	904272.81
42-7	122+60.56	32.25' LT	870.70	477370.62	904280.41
42-8	122+47.56	32.17' LT	870.68	477357.93	904277.62
42-10	122+47.59	27.17' LT	870.67	477356.86	904282.50
42-11	122+57.56	32.24' LT	870.64	477367.69	904279.76
42-12	122+63.26	27.27' LT	870.61	477372.16	904285.86
42-13	122+52.56	32.20' LT	870.57	477362.81	904278.69
42-14	122+57.59	27.24' LT	870.57	477366.62	904284.65
42-15	122+52.59	27.20' LT	870.54	477361.74	904283.57
42-16	122+61.13	27.26' LT	870.53	477370.08	904285.40
42-17	122+57.64	19.35' LT	870.33	477364.93	904292.35
42-18	122+52.61	22.66' LT	870.31	477360.76	904288.01
42-19	122+57.62	21.99' LT	870.23	477365.50	904289.77
42-20	122+52.65	16.65' LT	870.21	477359.48	904293.88
42-21	122+50.00	15.98' LT	870.18	477356.74	904293.95
42-22	122+52.64	18.78' LT	870.12	477359.93	904291.80
42-23	122+49.71	17.96' LT	869.93	477356.90	904291.95
42-24	122+44.80	27.94' LT	870.74	477354.31	904281.14
42-25	122+47.59	26.67' LT	870.72	477356.75	904282.99
42-26	122+52.09	26.70' LT	870.75	477361.14	904283.95



TRUMAN DR - NW					
POINT	STA	OFFSET	ELEV	NORTHING	EASTING
43-1	122+80.89	38.84' LT	870.81	477391.91	904278.47
43-2	123+11.00	32.72' LT	872.54	477419.93	904291.08
43-3	123+11.00	28.81' LT	872.50	477419.07	904294.89
43-4	123+05.87	27.54' LT	872.06	477413.79	904295.00
43-5	122+95.84	32.47' LT	871.28	477405.09	904287.97
43-6	122+95.87	27.47' LT	871.21	477404.02	904292.86
43-7	122+89.84	32.44' LT	871.17	477399.23	904286.69
43-8	122+89.87	27.44' LT	871.11	477398.16	904291.57
43-9	122+78.90	38.57' LT	870.84	477389.92	904278.29
43-10	122+79.39	35.00' LT	870.79	477389.60	904281.88
43-11	122+79.86	32.37' LT	870.76	477389.48	904284.55
43-12	122+82.35	24.43' LT	870.76	477390.16	904292.84
43-13	122+81.37	35.31' LT	870.73	477391.60	904282.01
43-14	122+83.84	32.40' LT	870.72	477393.37	904285.40
43-15	122+81.23	27.38' LT	870.70	477389.72	904289.72
43-16	122+81.90	32.39' LT	870.69	477391.48	904284.98
43-17	122+83.34	27.40' LT	870.62	477391.78	904290.17
43-18	122+84.19	25.21' LT	870.59	477392.13	904292.49

TRUMAN DR - SE					
POINT	STA	OFFSET	ELEV	NORTHING	EASTING
41-1	122+52.96	33.13' RT	871.05	477348.80	904342.51
41-2	122+68.88	33.14' RT	871.71	477364.32	904346.03
41-3	122+64.90	33.13' RT	871.63	477360.44	904345.14
41-4	122+68.88	29.31' RT	871.60	477365.17	904342.29
41-5	122+64.91	28.49' RT	871.56	477361.47	904340.62
41-6	122+62.91	28.08' RT	871.41	477359.61	904339.77
41-7	122+47.88	33.13' RT	871.35	477343.84	904341.38
41-8	122+47.88	29.30' RT	871.23	477344.69	904337.65
41-9	122+57.96	33.14' RT	871.10	477353.68	904343.61
41-10	122+57.93	28.14' RT	871.03	477354.75	904338.73
41-11	122+52.93	28.13' RT	870.97	477349.87	904337.62
41-12	122+60.86	16.22' RT	870.50	477360.23	904327.75
41-13	122+57.86	16.22' RT	870.45	477357.30	904327.10
41-14	122+57.87	18.22' RT	870.37	477356.87	904329.05
41-15	122+60.87	18.22' RT	870.36	477359.80	904329.70
41-16	122+52.86	16.24' RT	870.36	477352.42	904326.00
41-17	122+49.86	16.24' RT	870.31	477349.50	904325.35
41-18	122+52.87	18.24' RT	870.28	477351.99	904327.96
41-19	122+49.87	18.24' RT	870.17	477349.07	904327.30

PROJECT NO: 3070-04-60

HWY: STH 73

COUNTY: DANE

PAVING DETAILS - CURB RAMPS

SHEET **E**FILE NAME: Y:\50XX\5085_DP.STH73.DAN\CADDS\30700460\Sheets\PLAN\021209-PD.DWG
LAYOUT NAME - 021210-PD_Truman

PLOT DATE: 7/29/2022 1:43 PM

PLOT BY: HARRY TRAN

PLOT NAME:

PLOT SCALE: 1 IN=10 FT

WISDOT/CADDS SHEET 42

NORTH ST - SW					
POINT	STA	OFFSET	ELEV	NORTHING	EASTING
44-1	124+23.94	32.54' LT	873.436	477530.05	904316.16
44-2	124+12.94	32.53' LT	873.647	477519.32	904313.75
44-3	124+17.94	32.53' LT	873.551	477524.20	904314.84
44-4	124+12.95	28.66' LT	873.540	477518.47	904317.52
44-5	124+27.50	27.55' LT	873.482	477532.42	904321.81
44-6	124+17.95	27.53' LT	873.471	477523.10	904319.72
44-7	124+32.54	34.35' LT	873.466	477538.84	904316.29
44-8	124+21.60	22.46' LT	873.464	477525.55	904325.48
44-9	124+31.44	32.55' LT	873.460	477537.36	904317.80
44-10	124+24.68	27.54' LT	873.380	477529.67	904321.20
44-11	124+29.01	32.55' LT	873.360	477535.00	904317.27
44-12	124+30.79	35.31' LT	873.350	477537.34	904314.97
44-13	124+20.50	24.13' LT	873.315	477524.84	904323.61

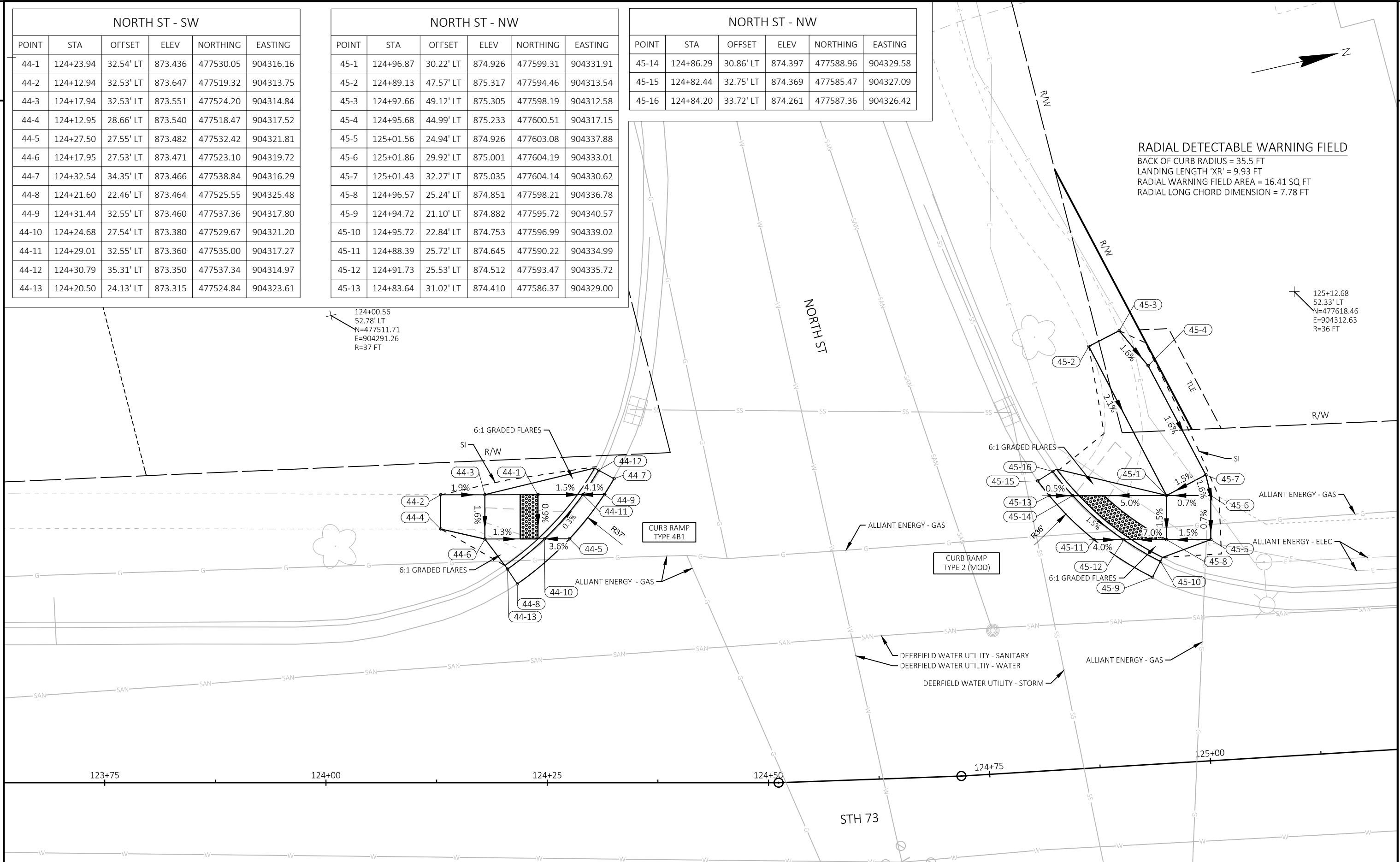
NORTH ST - NW					
POINT	STA	OFFSET	ELEV	NORTHING	EASTING
45-1	124+96.87	30.22' LT	874.926	477599.31	904331.91
45-2	124+89.13	47.57' LT	875.317	477594.46	904313.54
45-3	124+92.66	49.12' LT	875.305	477598.19	904312.58
45-4	124+95.68	44.99' LT	875.233	477600.51	904317.15
45-5	125+01.56	24.94' LT	874.926	477603.08	904337.88
45-6	125+01.86	29.92' LT	875.001	477604.19	904333.01
45-7	125+01.43	32.27' LT	875.035	477604.14	904330.62
45-8	124+96.57	25.24' LT	874.851	477598.21	904336.78
45-9	124+94.72	21.10' LT	874.882	477595.72	904340.57
45-10	124+95.72	22.84' LT	874.753	477596.99	904339.02
45-11	124+88.39	25.72' LT	874.645	477590.22	904334.99
45-12	124+91.73	25.53' LT	874.512	477593.47	904335.72
45-13	124+83.64	31.02' LT	874.410	477586.37	904329.00

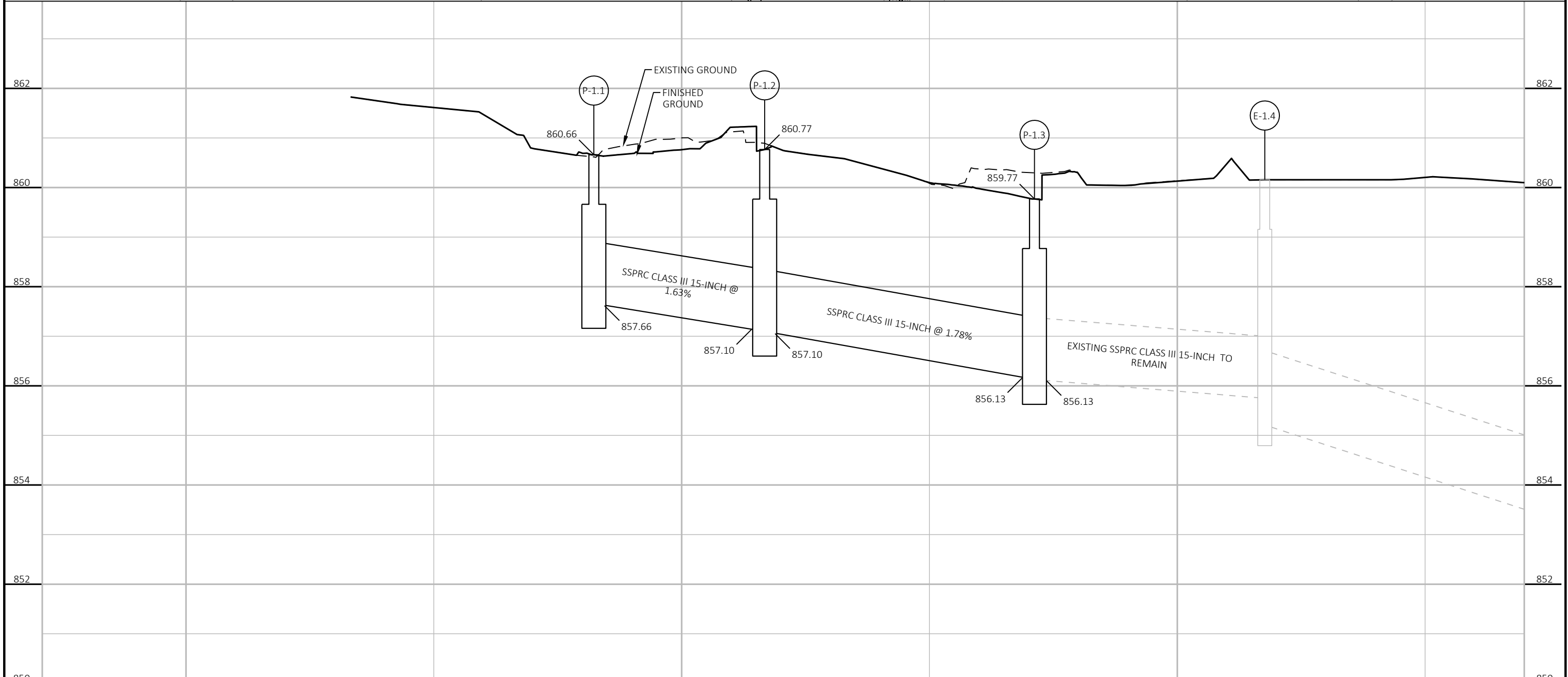
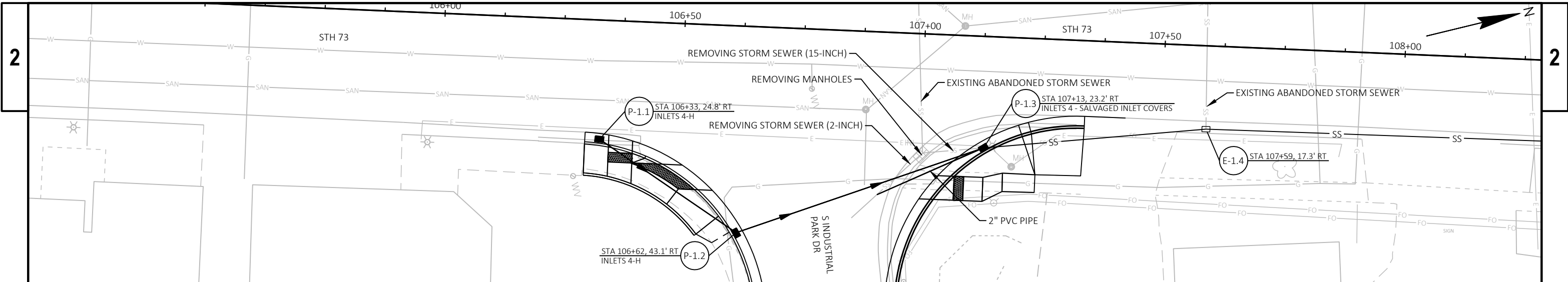
NORTH ST - NW					
POINT	STA	OFFSET	ELEV	NORTHING	EASTING
45-14	124+86.29	30.86' LT	874.397	477588.96	904329.58
45-15	124+82.44	32.75' LT	874.369	477585.47	904327.09
45-16	124+84.20	33.72' LT	874.261	477587.36	904326.42

RADIAL DETECTABLE WARNING FIELD
 BACK OF CURB RADIUS = 35.5 FT
 LANDING LENGTH 'XR' = 9.93 FT
 RADIAL WARNING FIELD AREA = 16.41 SQ FT
 RADIAL LONG CHORD DIMENSION = 7.78 FT

124+00.56
 52.78' LT
 N=477511.71
 E=904291.26
 R=37 FT

125+12.68
 52.33' LT
 N=477618.46
 E=904312.63
 R=36 FT

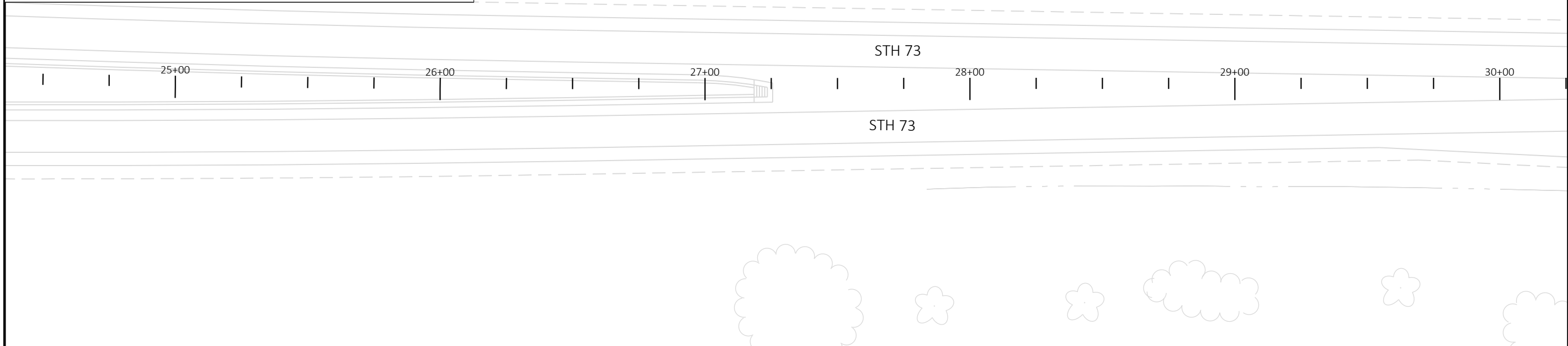




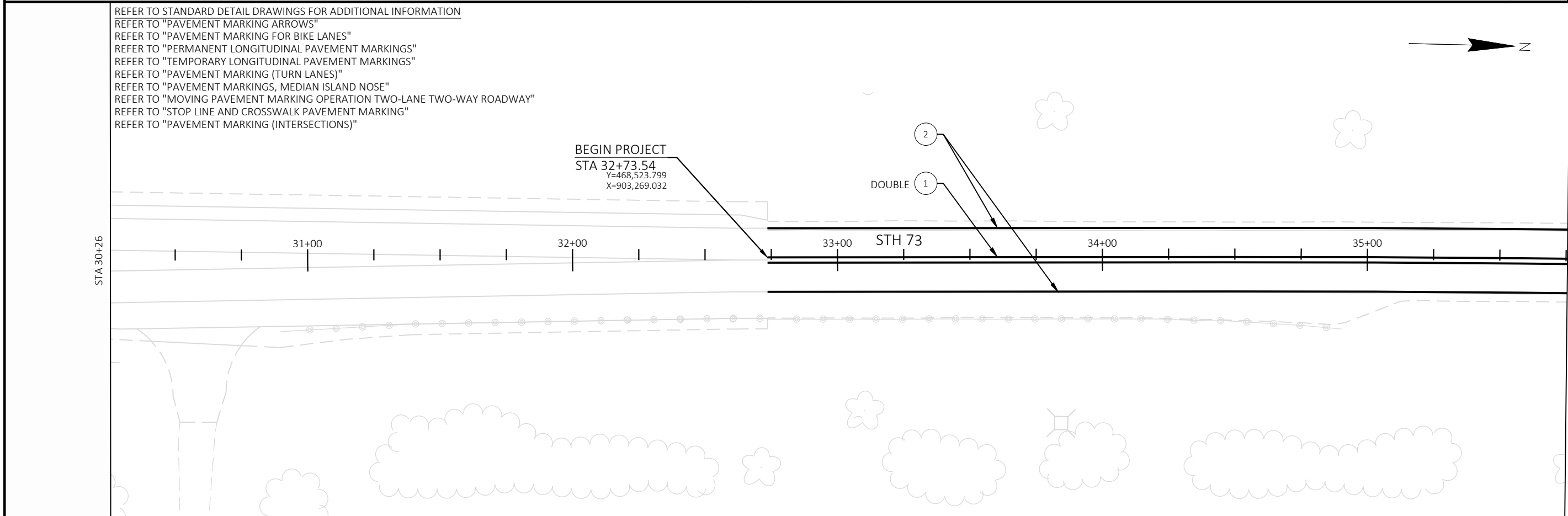
PROJECT NO: 3070-04-60	HWY: STH 73	COUNTY: DANE	STORM SEWER	SHEET	E
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LEGEND

- 1 MARKING LINE EPOXY 4-INCH (YELLOW)
- 2 MARKING LINE GROOVED WET REFLECTIVE EPOXY 4-INCH (WHITE)
- 3 MARKING STOP LINE EPOXY 18-INCH (WHITE)
- 4 MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)
- 5 MARKING PARKING STALL EPOXY (WHITE)
- 6 MARKING ISLAND NOSE EPOXY (YELLOW)
- 7 MARKING CURB EPOXY (YELLOW)
- 8 MARKING SYMBOL EPOXY (WHITE)



REFER TO STANDARD DETAIL DRAWINGS FOR ADDITIONAL INFORMATION
 REFER TO "PAVEMENT MARKING ARROWS"
 REFER TO "PAVEMENT MARKING FOR BIKE LANES"
 REFER TO "PERMANENT LONGITUDINAL PAVEMENT MARKINGS"
 REFER TO "TEMPORARY LONGITUDINAL PAVEMENT MARKINGS"
 REFER TO "PAVEMENT MARKING (TURN LANES)"
 REFER TO "PAVEMENT MARKINGS, MEDIAN ISLAND NOSE"
 REFER TO "MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY"
 REFER TO "STOP LINE AND CROSSWALK PAVEMENT MARKING"
 REFER TO "PAVEMENT MARKING (INTERSECTIONS)"



LEGEND

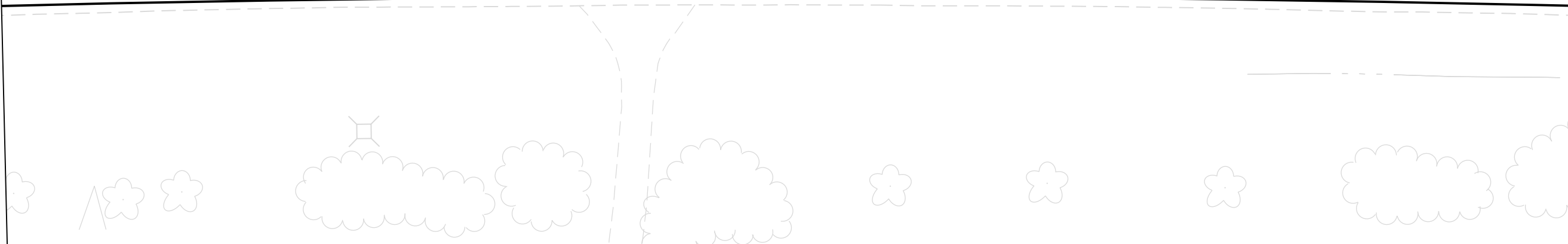
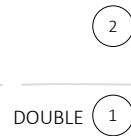
- ① MARKING LINE EPOXY 4-INCH (YELLOW)
- ② MARKING LINE GROOVED WET REFLECTIVE EPOXY 4-INCH (WHITE)
- ③ MARKING STOP LINE EPOXY 18-INCH (WHITE)
- ④ MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)
- ⑤ MARKING PARKING STALL EPOXY (WHITE)
- ⑥ MARKING ISLAND NOSE EPOXY (YELLOW)
- ⑦ MARKING CURB EPOXY (YELLOW)
- ⑧ MARKING SYMBOL EPOXY (WHITE)

STA 35+76

STA 41+26

36+00 37+00 38+00 39+00 40+00 41+00

STH 73

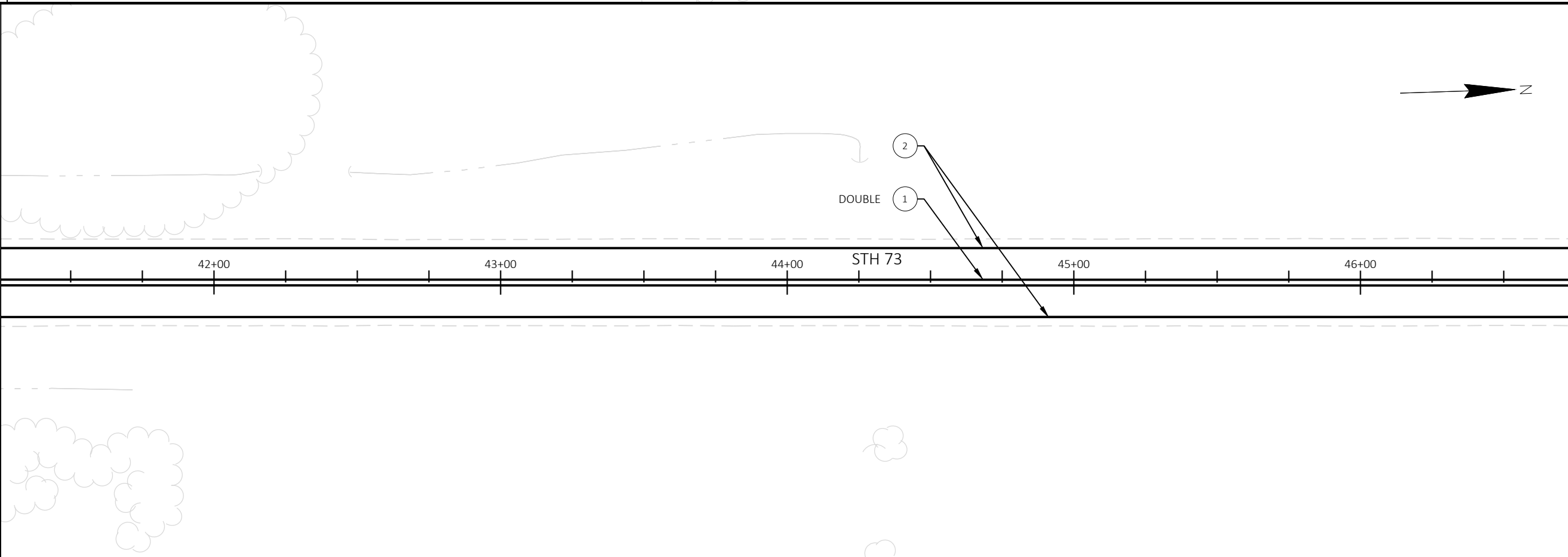
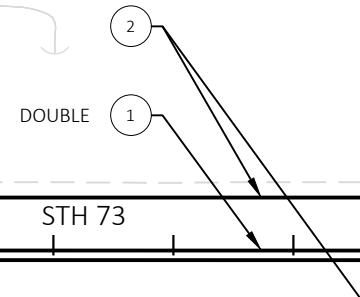


STA 41+26

STA 46+76

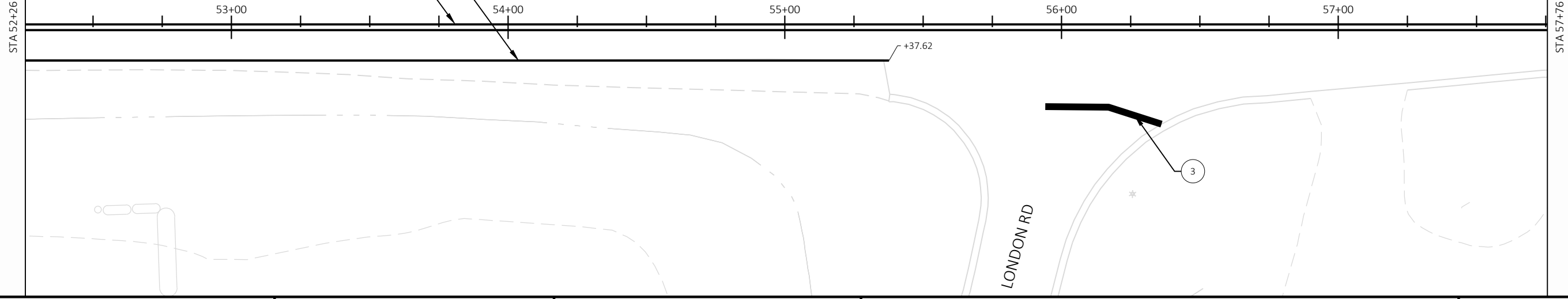
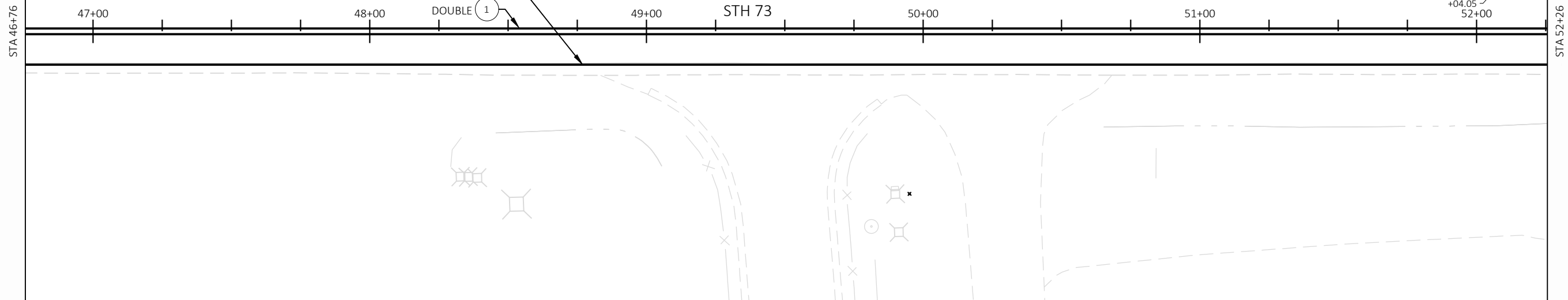
42+00 43+00 44+00 45+00 46+00

STH 73



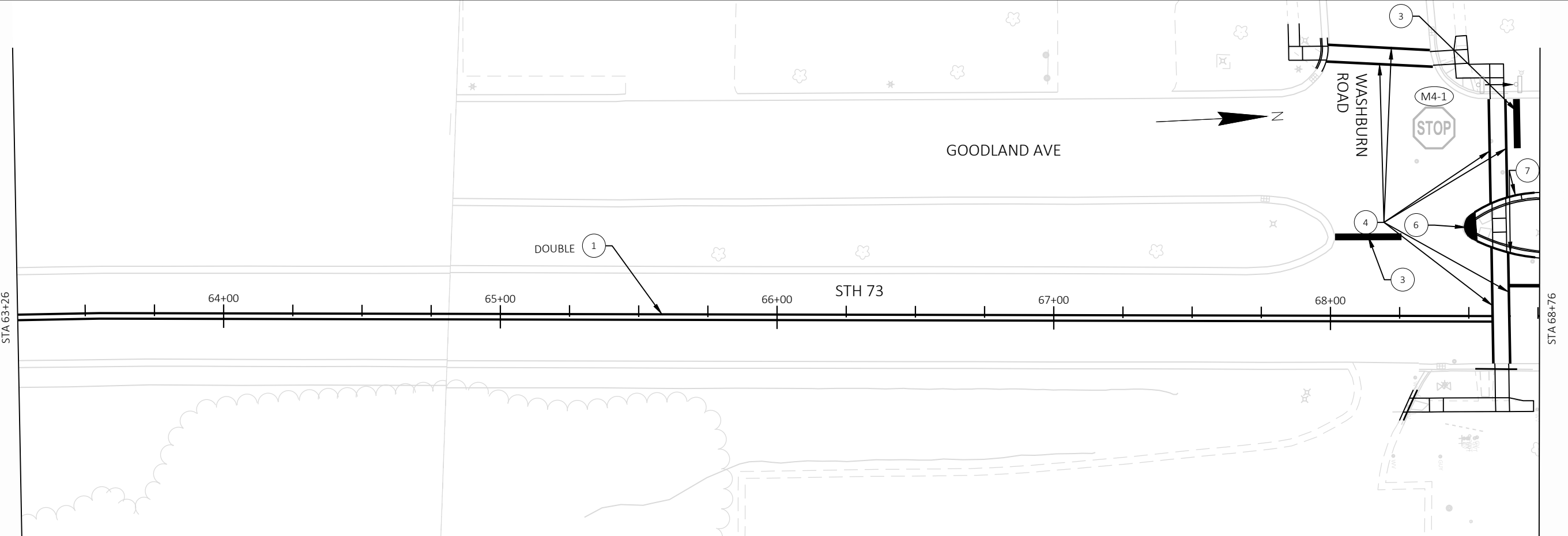
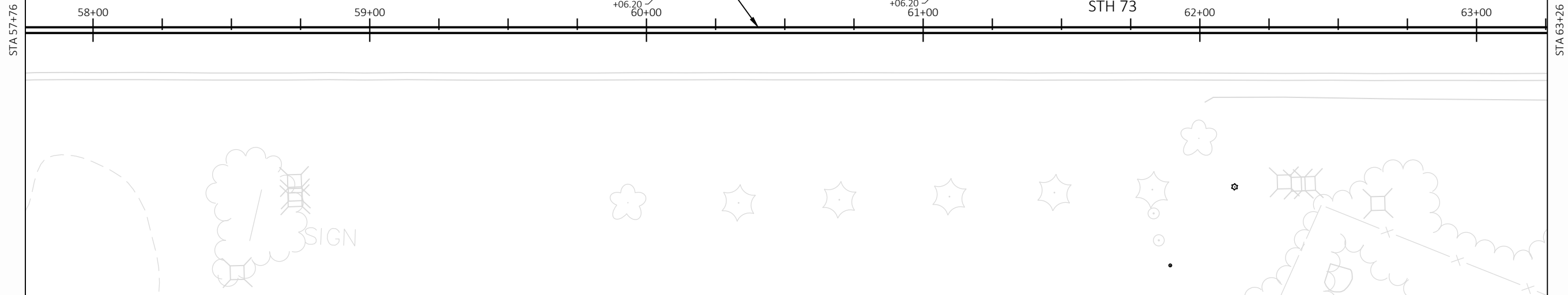
LEGEND

- ① MARKING LINE EPOXY 4-INCH (YELLOW)
- ② MARKING LINE GROOVED WET REFLECTIVE EPOXY 4-INCH (WHITE)
- ③ MARKING STOP LINE EPOXY 18-INCH (WHITE)
- ④ MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)
- ⑤ MARKING PARKING STALL EPOXY (WHITE)
- ⑥ MARKING ISLAND NOSE EPOXY (YELLOW)
- ⑦ MARKING CURB EPOXY (YELLOW)
- ⑧ MARKING SYMBOL EPOXY (WHITE)



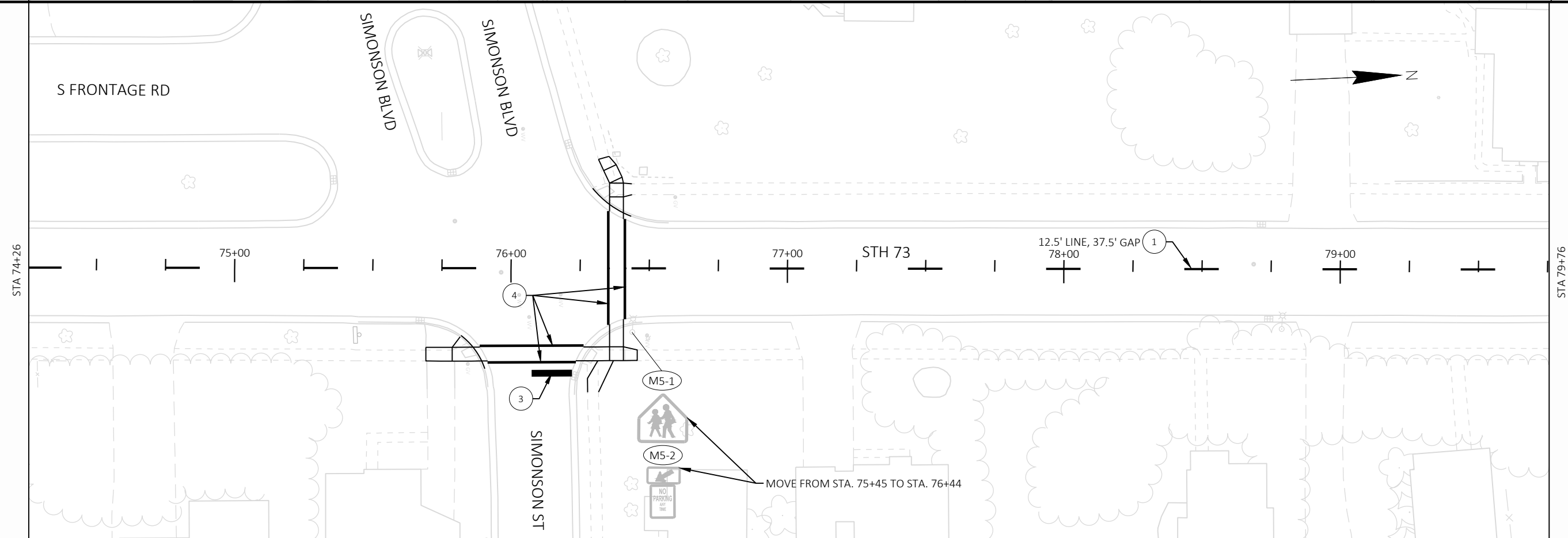
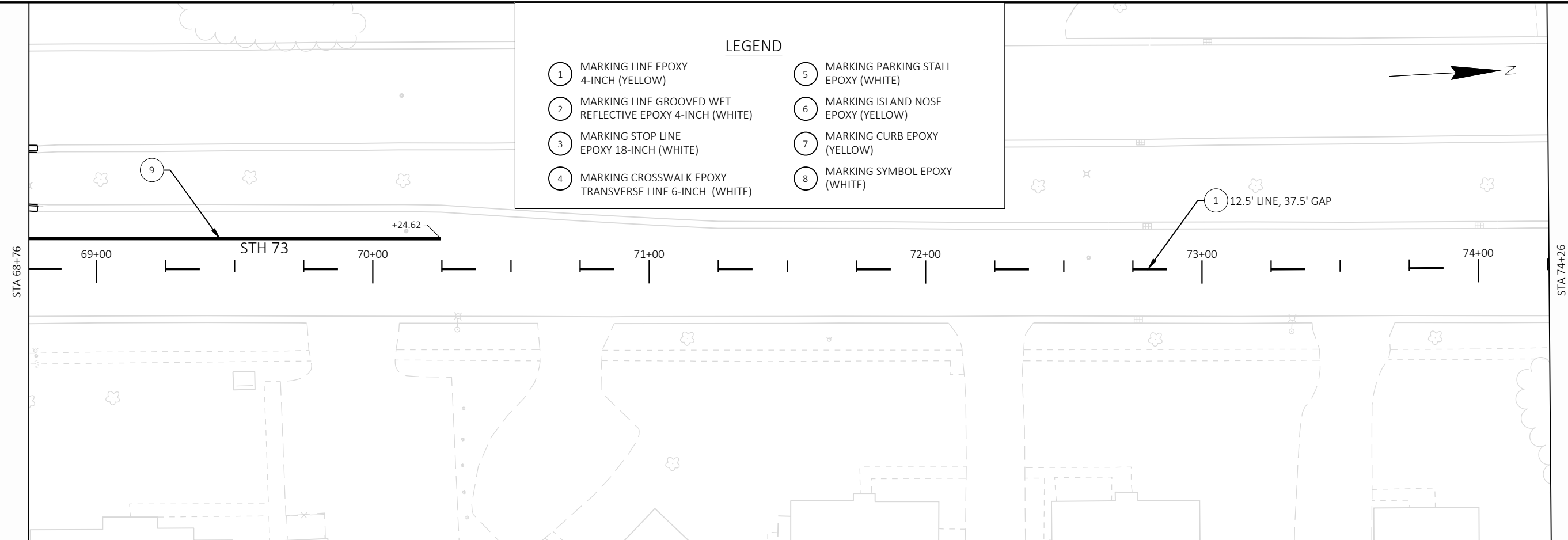
LEGEND

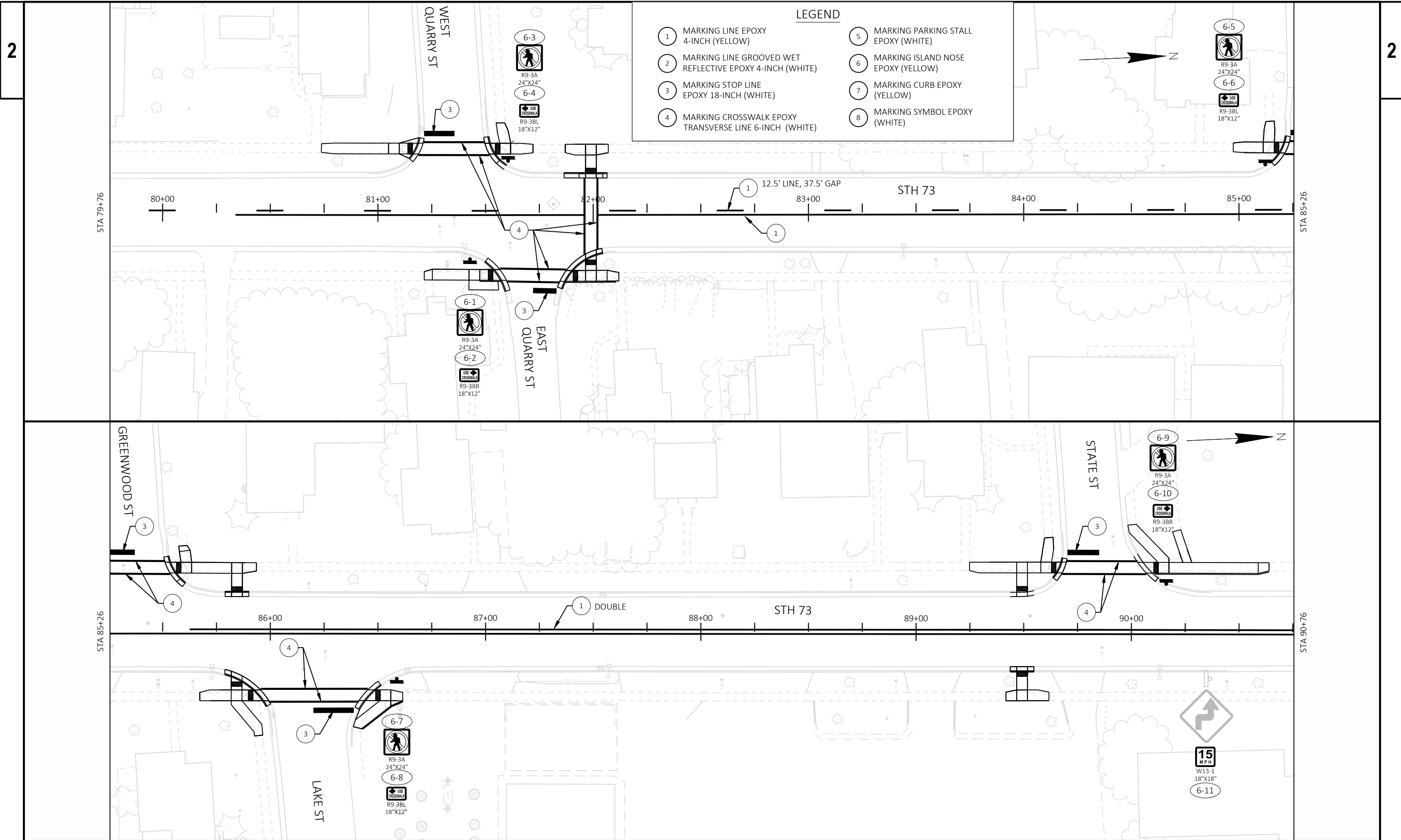
- ① MARKING LINE EPOXY 4-INCH (YELLOW)
- ② MARKING LINE GROOVED WET REFLECTIVE EPOXY 4-INCH (WHITE)
- ③ MARKING STOP LINE EPOXY 18-INCH (WHITE)
- ④ MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)
- ⑤ MARKING PARKING STALL EPOXY (WHITE)
- ⑥ MARKING ISLAND NOSE EPOXY (YELLOW)
- ⑦ MARKING CURB EPOXY (YELLOW)
- ⑧ MARKING SYMBOL EPOXY (WHITE)



LEGEND

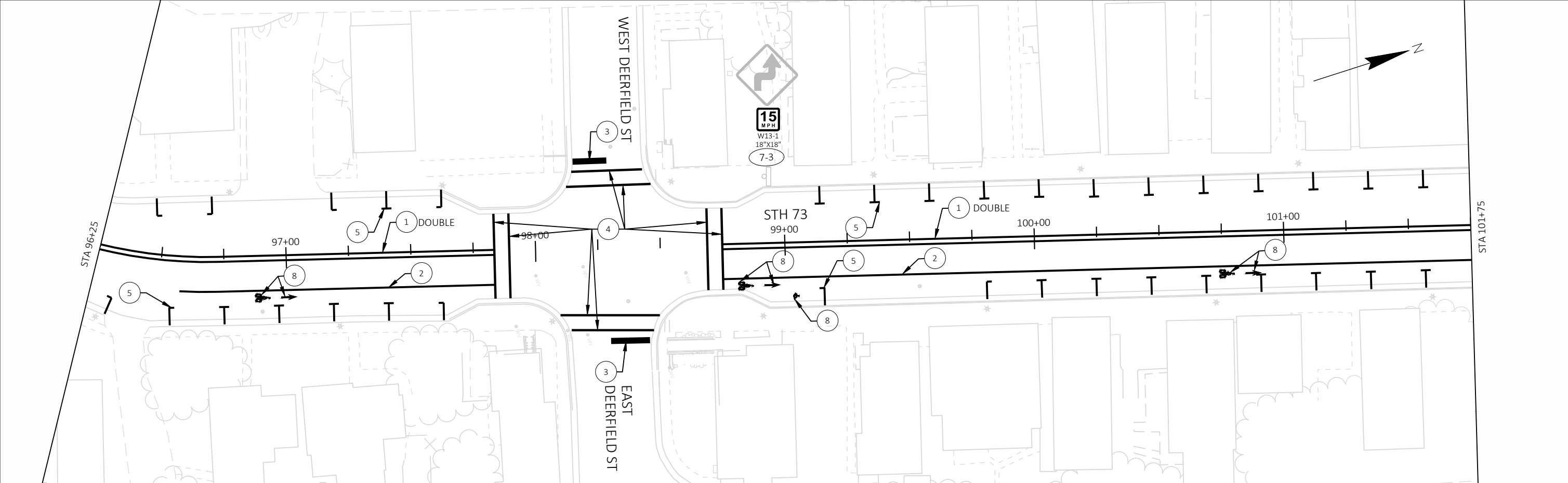
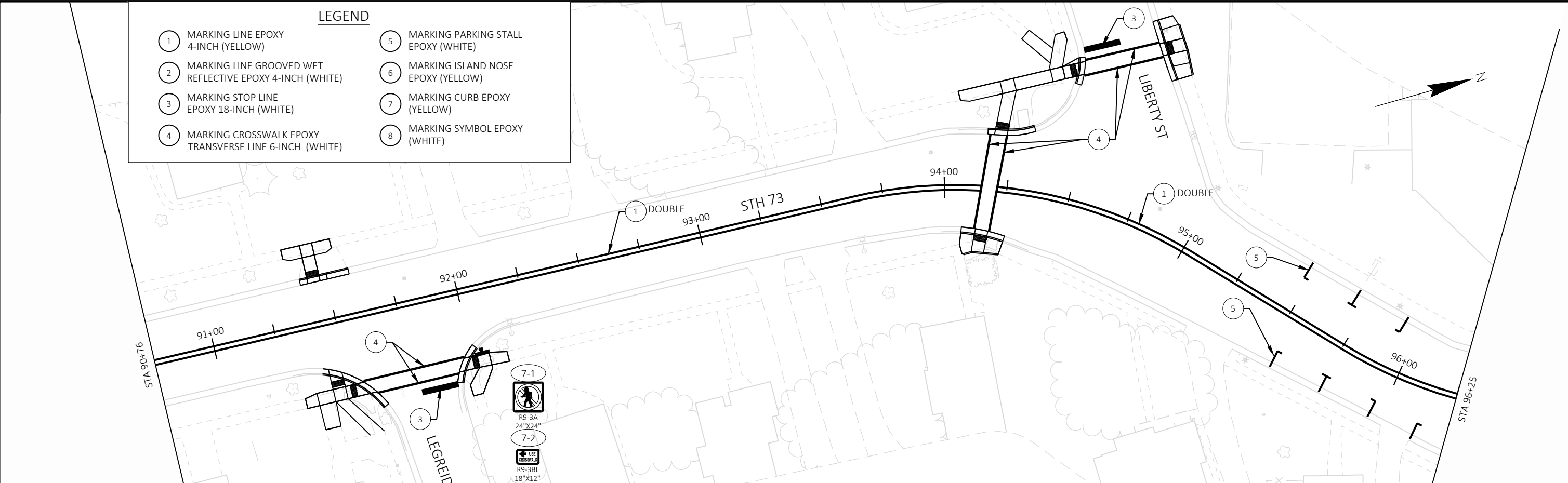
- ① MARKING LINE EPOXY 4-INCH (YELLOW)
- ② MARKING LINE GROOVED WET REFLECTIVE EPOXY 4-INCH (WHITE)
- ③ MARKING STOP LINE EPOXY 18-INCH (WHITE)
- ④ MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)
- ⑤ MARKING PARKING STALL EPOXY (WHITE)
- ⑥ MARKING ISLAND NOSE EPOXY (YELLOW)
- ⑦ MARKING CURB EPOXY (YELLOW)
- ⑧ MARKING SYMBOL EPOXY (WHITE)

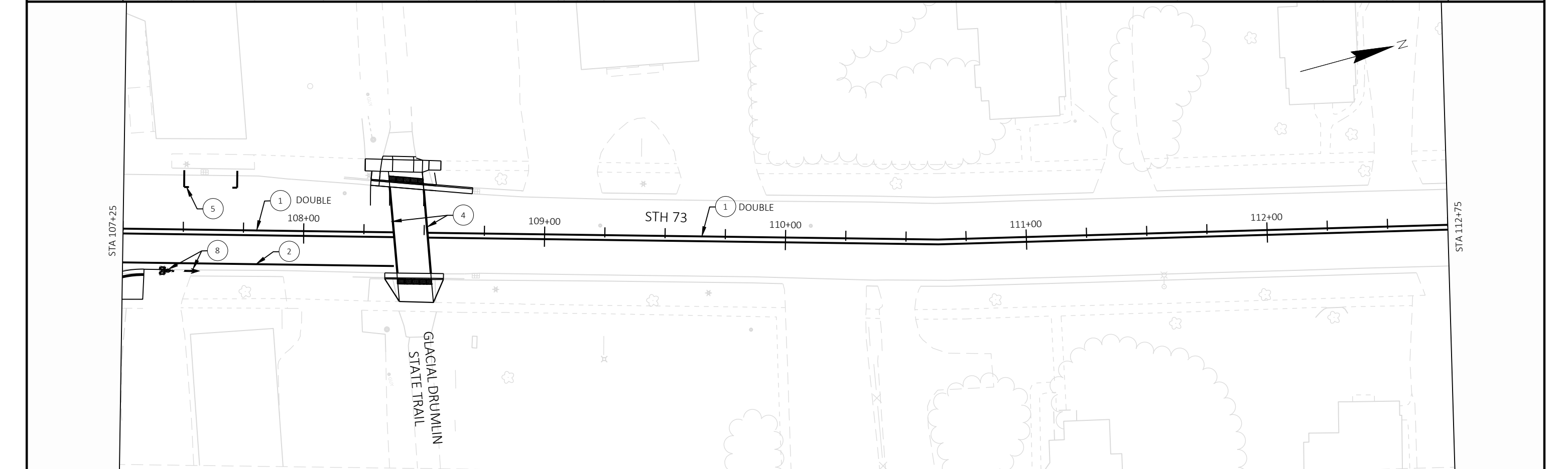
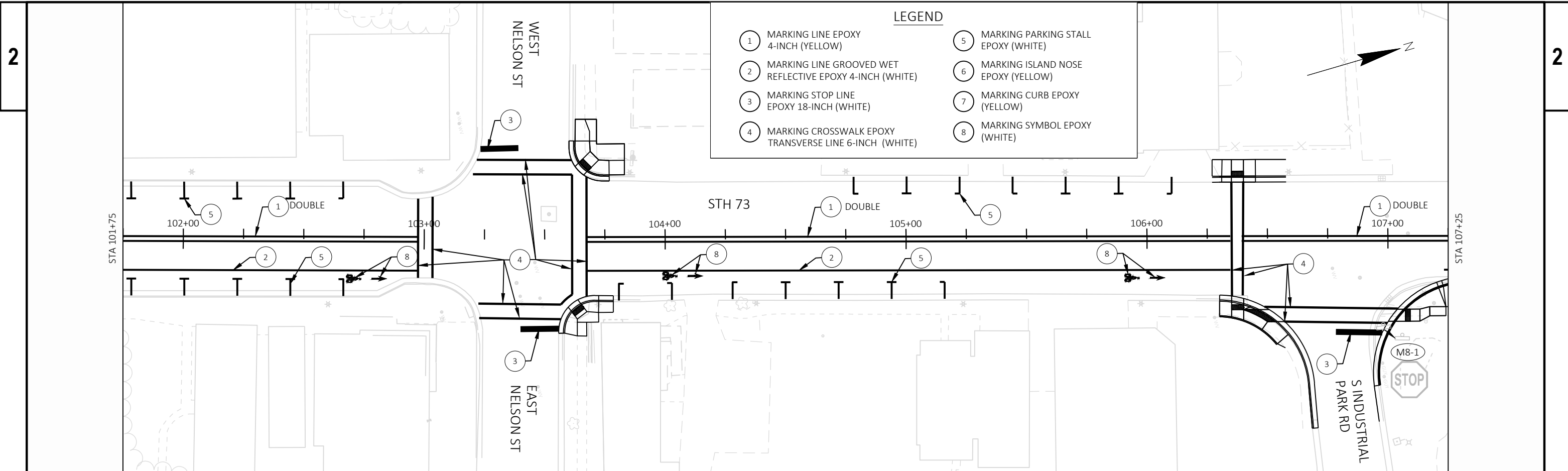




LEGEND

1 MARKING LINE EPOXY 4-INCH (YELLOW)	5 MARKING PARKING STALL EPOXY (WHITE)
2 MARKING LINE GROOVED WET REFLECTIVE EPOXY 4-INCH (WHITE)	6 MARKING ISLAND NOSE EPOXY (YELLOW)
3 MARKING STOP LINE EPOXY 18-INCH (WHITE)	7 MARKING CURB EPOXY (YELLOW)
4 MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)	8 MARKING SYMBOL EPOXY (WHITE)





LEGEND

- 1 MARKING LINE EPOXY 4-INCH (YELLOW)
- 2 MARKING LINE GROOVED WET REFLECTIVE EPOXY 4-INCH (WHITE)
- 3 MARKING STOP LINE EPOXY 18-INCH (WHITE)
- 4 MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)
- 5 MARKING PARKING STALL EPOXY (WHITE)
- 6 MARKING ISLAND NOSE EPOXY (YELLOW)
- 7 MARKING CURB EPOXY (YELLOW)
- 8 MARKING SYMBOL EPOXY (WHITE)

STA 112+75

STA 118+25



113+00 114+00 115+00 116+00 117+00 118+00

1 DOUBLE

STH 73

STA 118+25

STA 123+75



119+00 120+00 121+00 122+00 123+00

1 DOUBLE

1 DOUBLE

STH 73

TRUMAN DR

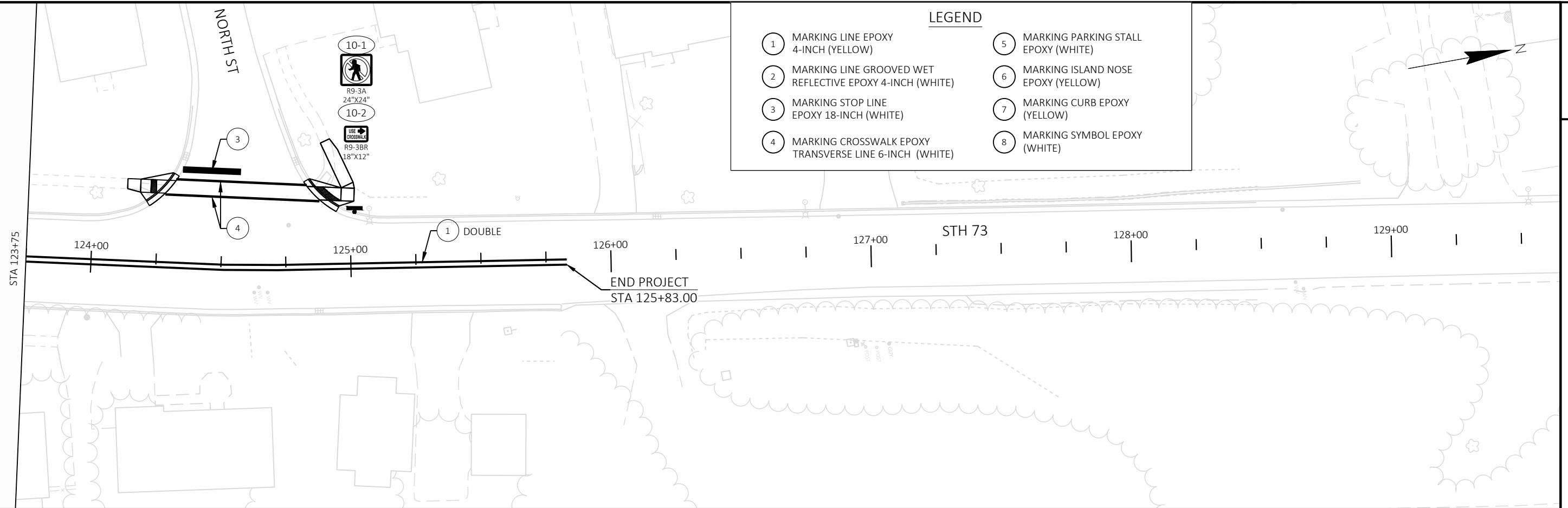
3

4

N INDUSTRIAL PARK RD

3

4



LEGEND

- ① PLACE TRAFFIC CONTROL SIGNS PER "TYPICAL SIDEROAD APPROACH WARNING SIGN DETAIL" IN SDD "TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M.P.H. OR LESS, TWO WAY UNDIVIDED ROAD OPEN TO TRAFFIC".
- ② PLACE TRAFFIC CONTROL SIGNS PER "TYPICAL SIDEROAD APPROACH WARNING SIGN DETAIL" IN SDD "TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M.P.H. OR GREATER, TWO WAY UNDIVIDED ROAD OPEN TO TRAFFIC".
- ③ PLACE TRAFFIC CONTROL SIGNS PER "TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45MPH OR GREATER" IN SDD "TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M.P.H. OR GREATER, TWO WAY UNDIVIDED ROAD OPEN TO TRAFFIC".

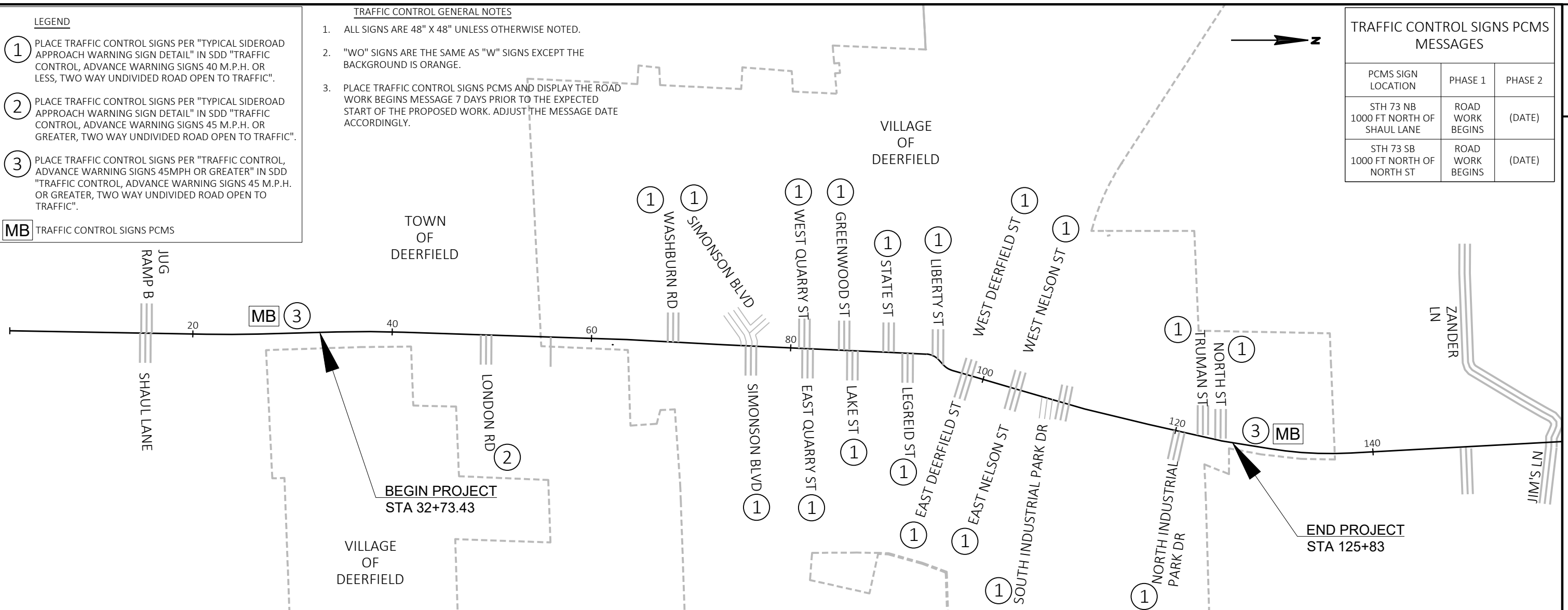
MB TRAFFIC CONTROL SIGNS PCMS

TRAFFIC CONTROL GENERAL NOTES

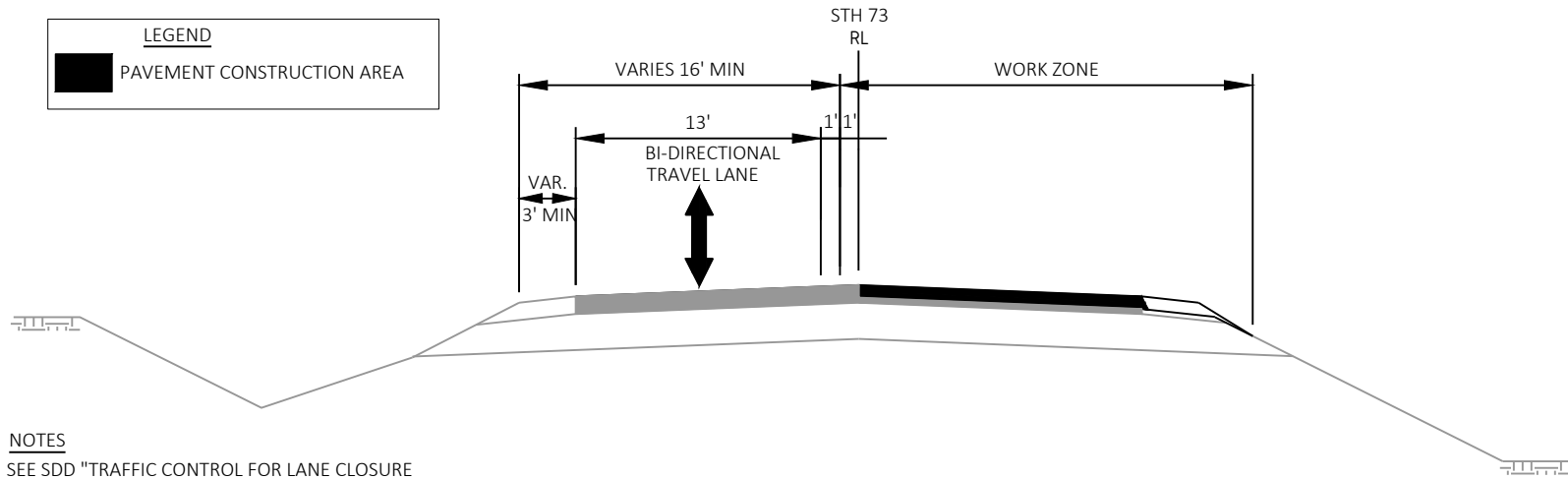
- 1. ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.
- 2. "WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.
- 3. PLACE TRAFFIC CONTROL SIGNS PCMS AND DISPLAY THE ROAD WORK BEGINS MESSAGE 7 DAYS PRIOR TO THE EXPECTED START OF THE PROPOSED WORK. ADJUST THE MESSAGE DATE ACCORDINGLY.

TRAFFIC CONTROL SIGNS PCMS MESSAGES

PCMS SIGN LOCATION	PHASE 1	PHASE 2
STH 73 NB 1000 FT NORTH OF SHAUL LANE	ROAD WORK BEGINS	(DATE)
STH 73 SB 1000 FT NORTH OF NORTH ST	ROAD WORK BEGINS	(DATE)



LEGEND
■ PAVEMENT CONSTRUCTION AREA



NOTES
SEE SDD "TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION" TO REDUCE TRAFFIC TO ONE LANE OF BI-DIRECTIONAL TRAFFIC DURING WORKING HOURS.
THE WORK ZONE AS SHOWN INCLUDES WORKING ROOM AND BUFFER TO LIVE TRAFFIC.
SAME DAY PAVING IS REQUIRED ON ALL MILLED SURFACES.

TRAFFIC CONTROL TYPICAL SECTION

STH 73
TYPICAL STAGING FOR MILL AND OVERLAY OPERATIONS (WORK ZONE FOR STH 73 NB SHOWN, MIRROR TYPICAL SECTION FOR STH 73 SB WORK ZONE)

LEGEND

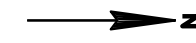
- ① PLACE TRAFFIC CONTROL SIGNS PER "TYPICAL SIDEROAD APPROACH WARNING SIGN DETAIL" IN SDD "TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M.P.H. OR LESS, TWO WAY UNDIVIDED ROAD OPEN TO TRAFFIC".
- ② PLACE TRAFFIC CONTROL SIGNS PER "TYPICAL SIDEROAD APPROACH WARNING SIGN DETAIL" IN SDD "TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M.P.H. OR GREATER, TWO WAY UNDIVIDED ROAD OPEN TO TRAFFIC".
- ③ PLACE TRAFFIC CONTROL SIGNS PER "TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45MPH OR GREATER" IN SDD "TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M.P.H. OR GREATER, TWO WAY UNDIVIDED ROAD OPEN TO TRAFFIC".
- ④ PLACE TRAFFIC CONTROL SIGNS PER "BARRICADES AND SIGNS FOR MAINLINE CLOSURES"
- ⑤ PLACE TRAFFIC CONTROL SIGNS PER "BARRICADES AND SIGNS FOR SIDEROAD CLOSURES"
- MB TRAFFIC CONTROL SIGNS PCMS

TRAFFIC CONTROL GENERAL NOTES

- 1. ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.
- 2. "WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.
- 3. PLACE TRAFFIC CONTROL SIGNS PCMS AND DISPLAY THE CLOSURE MESSAGE 7 DAYS PRIOR TO THE EXPECTED START OF THE ROAD CLOSURE. ADJUST THE MESSAGE DATE ACCORDINGLY.

TRAFFIC CONTROL SIGNS PCMS MESSAGES

PCMS SIGN LOCATION	PHASE 1	PHASE 2
STH 73 NB 1000 FT NORTH OF SHAUL LANE	ROAD CLOSURE BEGINS	(DATE)
STH 73 SB 1000 FT NORTH OF NORTH ST	ROAD CLOSURE BEGINS	(DATE)



DETOUR NOTES

THE EXACT LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ALL SIGNS INAPPROPRIATE TO THE STATUS OF THE TRAFFIC CONTROL PLAN, INCLUDING PRE-EXISTING SIGNING IN THE VICINITY, SHALL BE COVERED OR REMOVED AS SPECIFIED IN THE PLANS AND/OR AS DIRECTED BY THE ENGINEER.

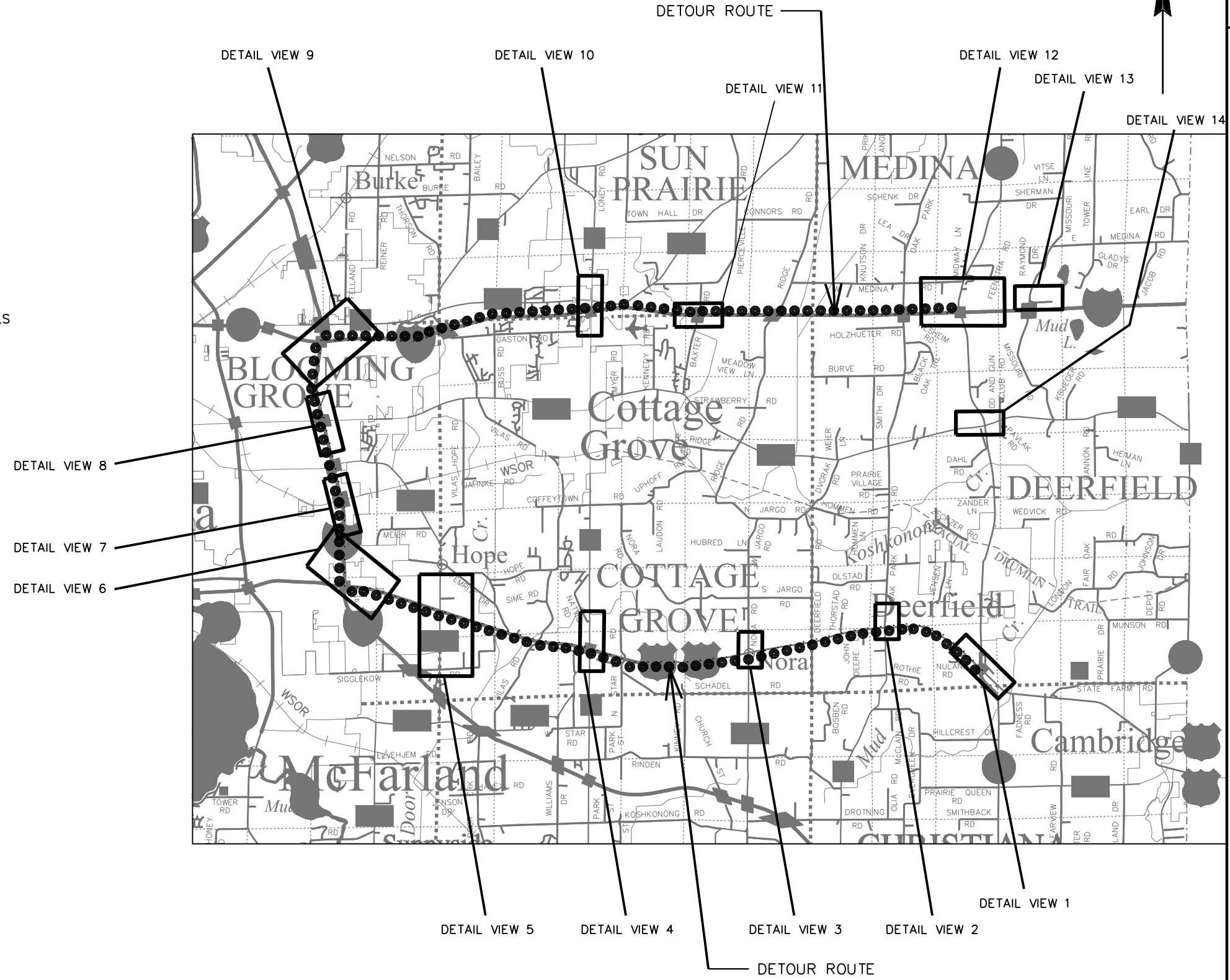
ALL EXISTING SIGNS THAT NEED TO BE COVERED SHALL BE COVERED WITH A BLANK ORANGE PANEL. PAID FOR AS TRAFFIC CONTROL COVERING SIGNS TYPE II UNLESS OTHERWISE NOTED.

ALL "W" AND "WO" SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THAT THE BACKGROUND IS ORANGE.

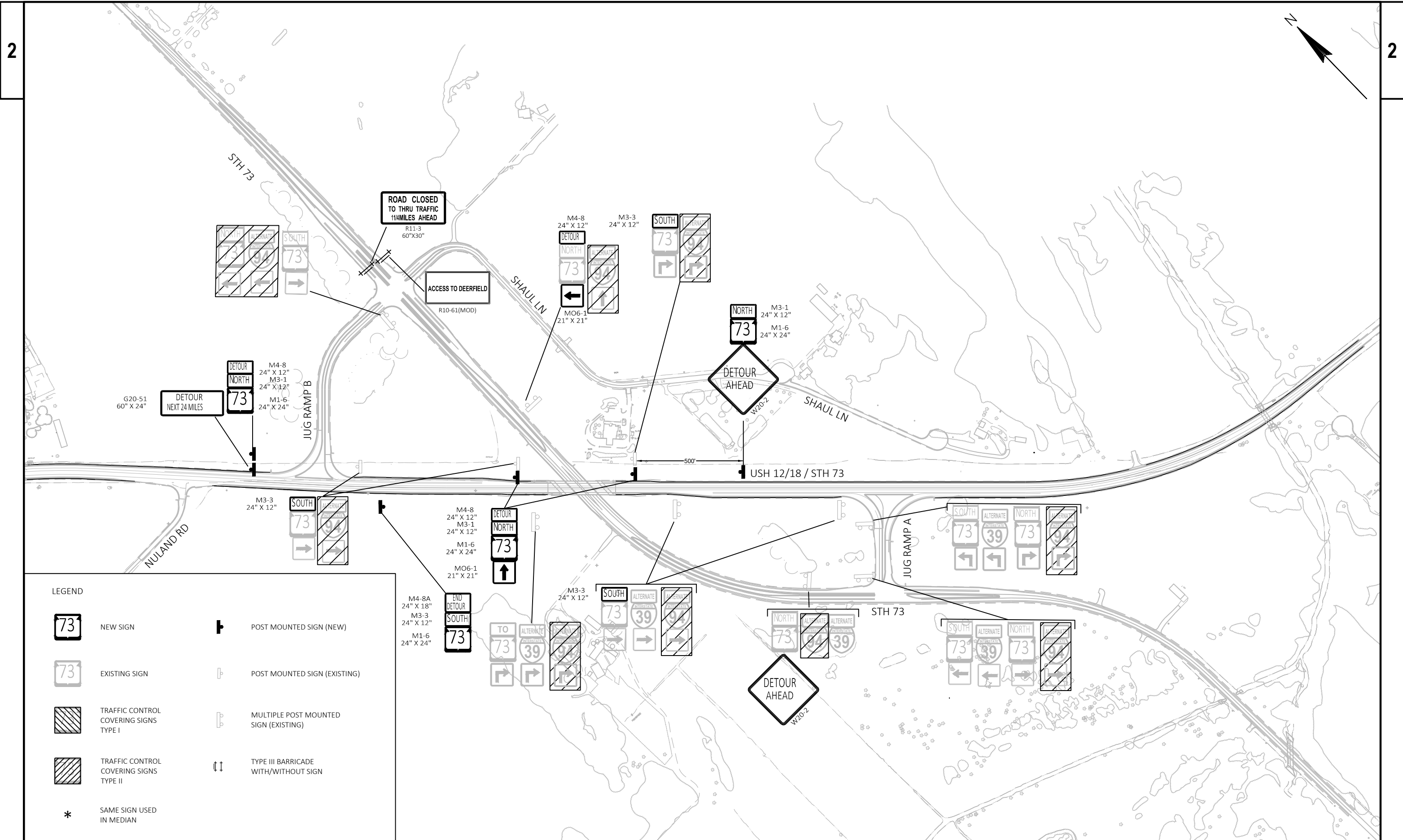
ALL "STOP" SIGNS THAT ARE REMOVED FOR A CONSTRUCTION OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED.

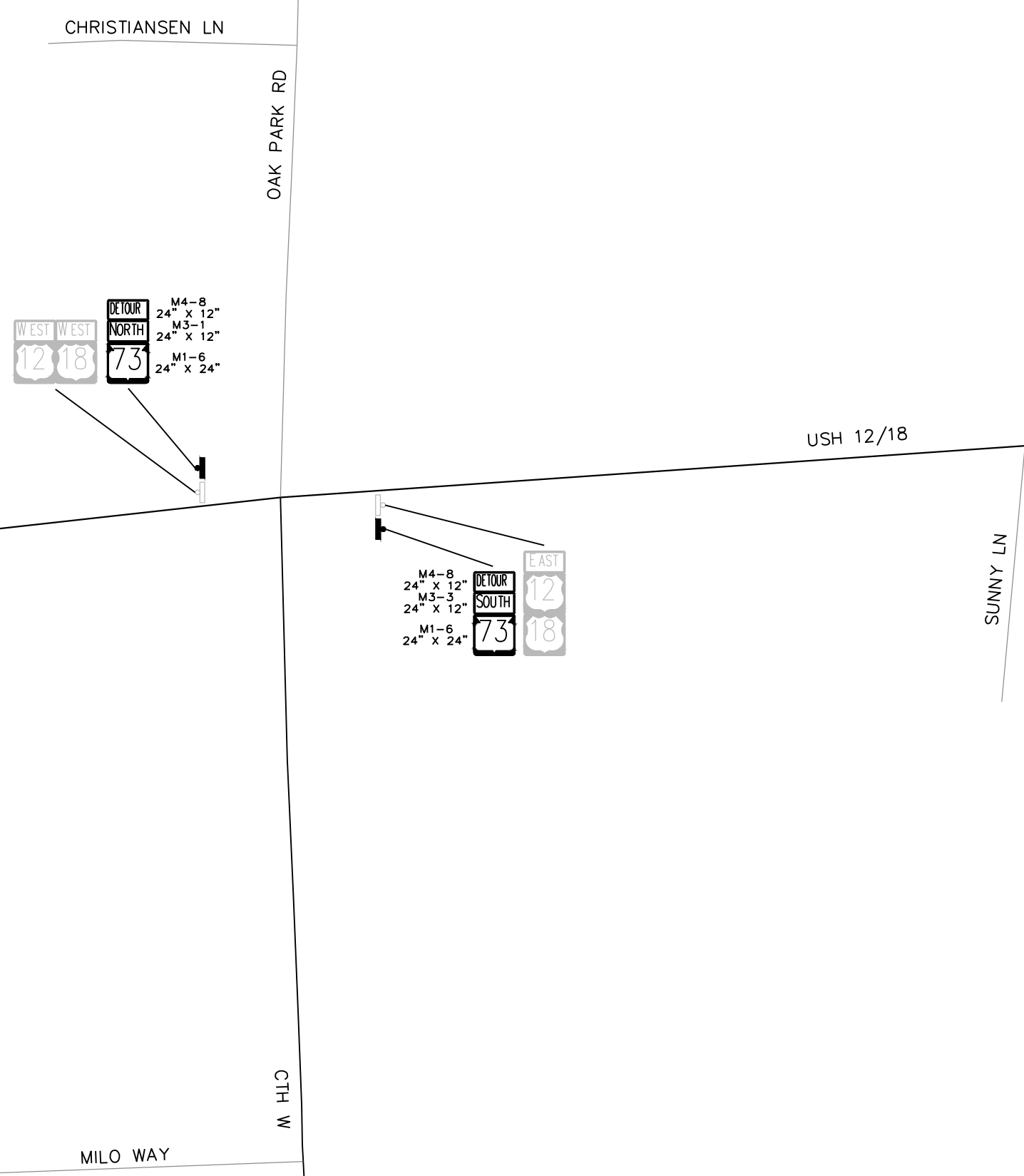
REFER TO SDD "BARRICADES AND SIGNS FOR MAINLINE CLOSURES" AND SDD "DETOUR SIGNING FOR MAINLINE CLOSURES" FOR SIGN SPACING NOT SHOWN IN DETAILS



LEGEND

	NEW SIGN		POST MOUNTED SIGN (NEW)
	EXISTING SIGN		POST MOUNTED SIGN (EXISTING)
	TRAFFIC CONTROL COVERING SIGNS TYPE I		MULTIPLE POST MOUNTED SIGN (EXISTING)
	TRAFFIC CONTROL COVERING SIGNS TYPE II		TYPE III BARRICADE WITH/WITHOUT SIGN
* SAME SIGN USED IN MEDIAN			





LEGEND

- | | | | |
|---|--|--|---------------------------------------|
| | NEW SIGN | | POST MOUNTED SIGN (NEW) |
| | EXISTING SIGN | | POST MOUNTED SIGN (EXISTING) |
| | TRAFFIC CONTROL COVERING SIGNS TYPE I | | MULTIPLE POST MOUNTED SIGN (EXISTING) |
| | TRAFFIC CONTROL COVERING SIGNS TYPE II | | TYPE III BARRICADE WITH/WITHOUT SIGN |
| * | SAME SIGN USED IN MEDIAN | | |

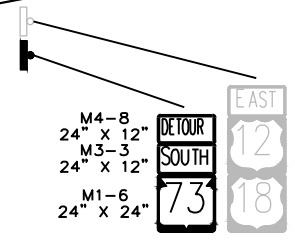
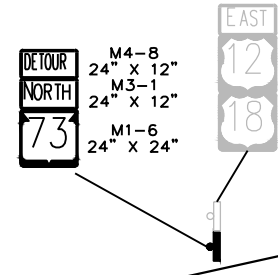


NORA RD

CTH BN

USH 12/18









USH 12/18

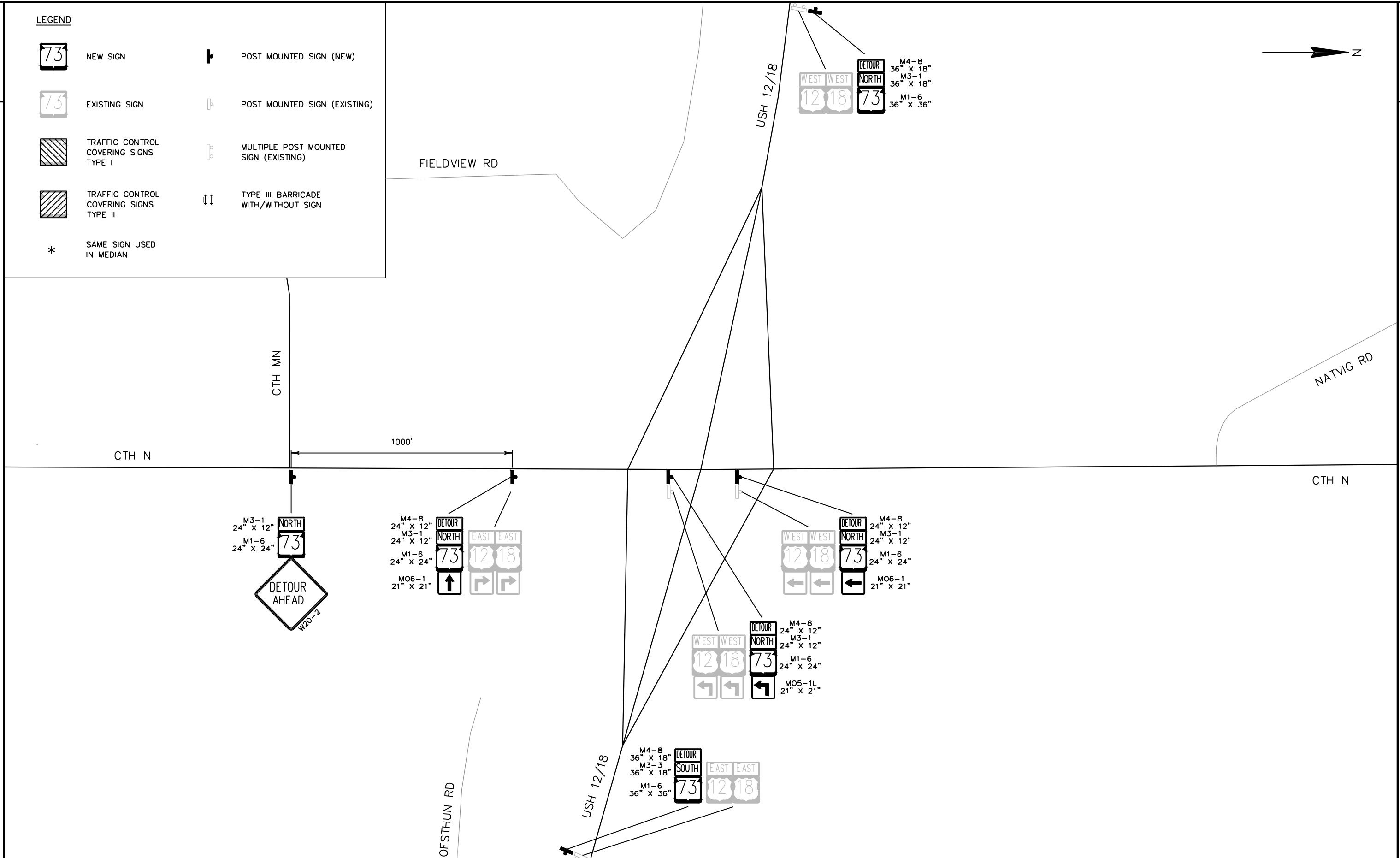


LEGEND










- NEW SIGN
- EXISTING SIGN
- TRAFFIC CONTROL COVERING SIGNS TYPE I
- TRAFFIC CONTROL COVERING SIGNS TYPE II
- POST MOUNTED SIGN (NEW)
- POST MOUNTED SIGN (EXISTING)
- MULTIPLE POST MOUNTED SIGN (EXISTING)
- TYPE III BARRICADE WITH/WITHOUT SIGN
- * SAME SIGN USED IN MEDIAN

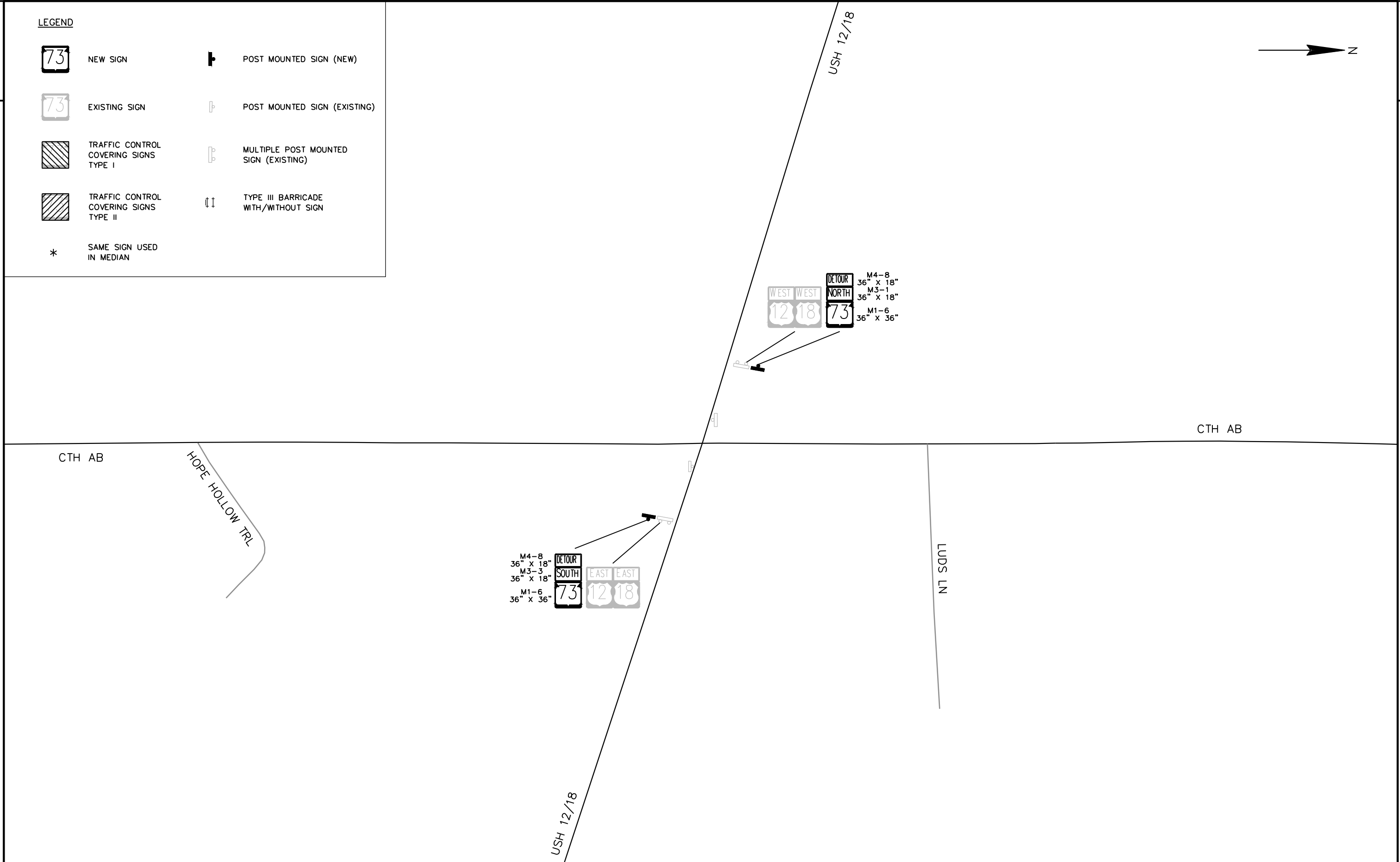
LEGEND

-  NEW SIGN
-  EXISTING SIGN
-  TRAFFIC CONTROL COVERING SIGNS TYPE I
-  TRAFFIC CONTROL COVERING SIGNS TYPE II
- * SAME SIGN USED IN MEDIAN
-  POST MOUNTED SIGN (NEW)
-  POST MOUNTED SIGN (EXISTING)
-  MULTIPLE POST MOUNTED SIGN (EXISTING)
-  TYPE III BARRICADE WITH/WITHOUT SIGN







LEGEND





-  NEW SIGN
-  EXISTING SIGN
-  TRAFFIC CONTROL COVERING SIGNS TYPE I
-  TRAFFIC CONTROL COVERING SIGNS TYPE II
-  * SAME SIGN USED IN MEDIAN
-  POST MOUNTED SIGN (NEW)
-  POST MOUNTED SIGN (EXISTING)
-  MULTIPLE POST MOUNTED SIGN (EXISTING)
-  TYPE III BARRICADE WITH/WITHOUT SIGN

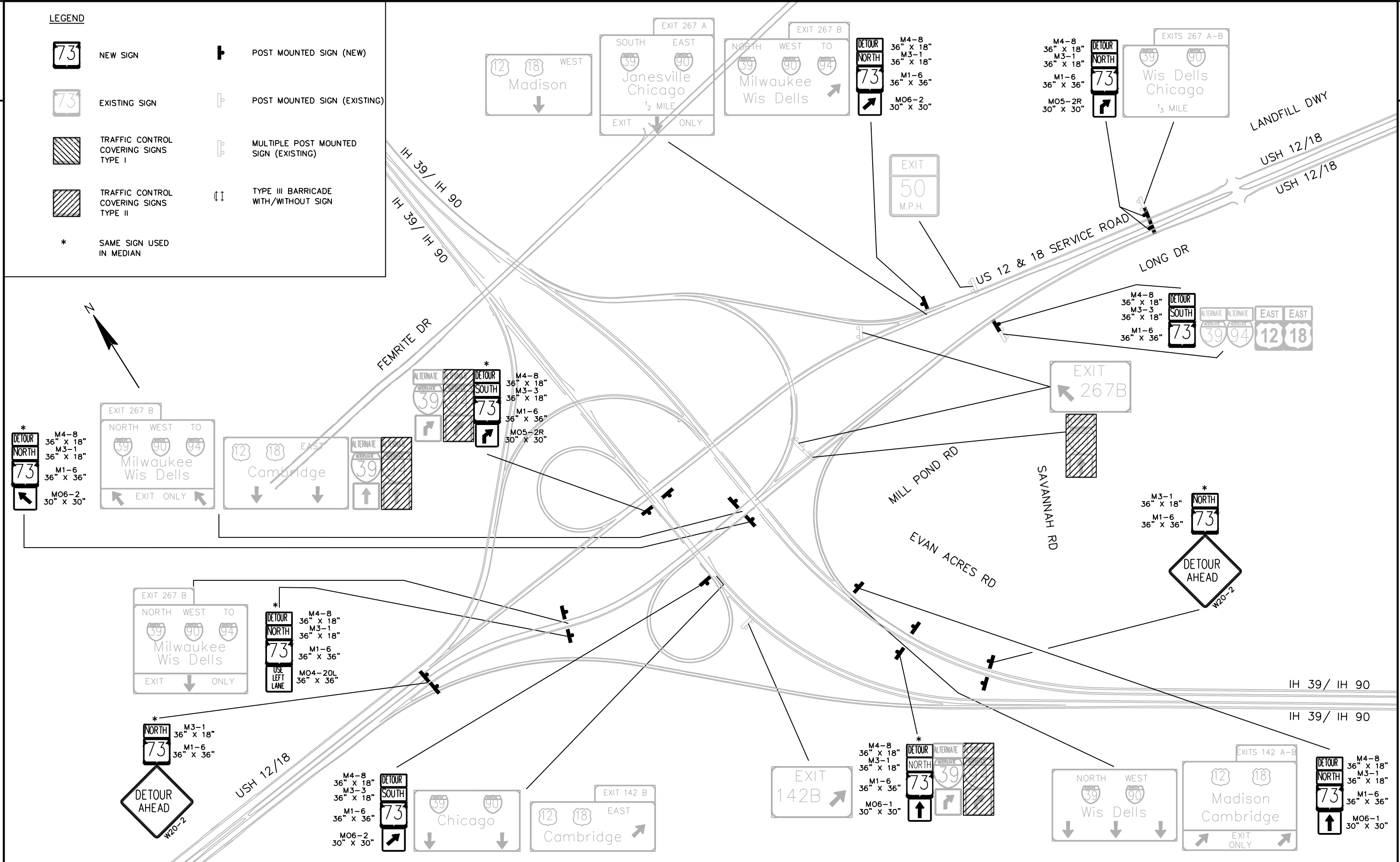


PROJECT NO: 3070-04-60	HWY: STH 73	COUNTY: DANE	DETOUR PLAN STH 73 - DETAIL VIEW 5	SHEET	E
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LEGEND

-  NEW SIGN
-  EXISTING SIGN
-  TRAFFIC CONTROL COVERING SIGNS TYPE I
-  TRAFFIC CONTROL COVERING SIGNS TYPE II
- *

 SAME SIGN USED IN MEDIAN
-  POST MOUNTED SIGN (NEW)
-  POST MOUNTED SIGN (EXISTING)
-  MULTIPLE POST MOUNTED SIGN (EXISTING)
-  TYPE III BARRICADE WITH/WITHOUT SIGN



LEGEND



NEW SIGN



POST MOUNTED SIGN (NEW)



EXISTING SIGN



POST MOUNTED SIGN (EXISTING)



TRAFFIC CONTROL COVERING SIGNS TYPE I



MULTIPLE POST MOUNTED SIGN (EXISTING)



TRAFFIC CONTROL COVERING SIGNS TYPE II



TYPE III BARRICADE WITH/WITHOUT SIGN

*

SAME SIGN USED IN MEDIAN



WI & SOUTHERN RAILROAD

CTH AB

IH 39 / IH 90

CTH AB

WI & SOUTHERN RAILROAD









IH 39 / IH 90

FOOT BRIDGE



M4-8
36" x 18"
M3-1
36" x 18"
M1-6
36" x 36"

LEGEND

-  NEW SIGN
-  EXISTING SIGN
-  TRAFFIC CONTROL COVERING SIGNS TYPE I
-  TRAFFIC CONTROL COVERING SIGNS TYPE II
- * SAME SIGN USED IN MEDIAN
-  POST MOUNTED SIGN (NEW)
-  POST MOUNTED SIGN (EXISTING)
-  MULTIPLE POST MOUNTED SIGN (EXISTING)
-  TYPE III BARRICADE WITH/WITHOUT SIGN

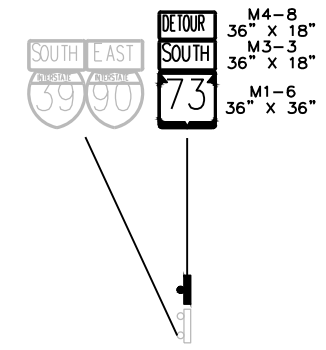


CTH BB









CTH BB

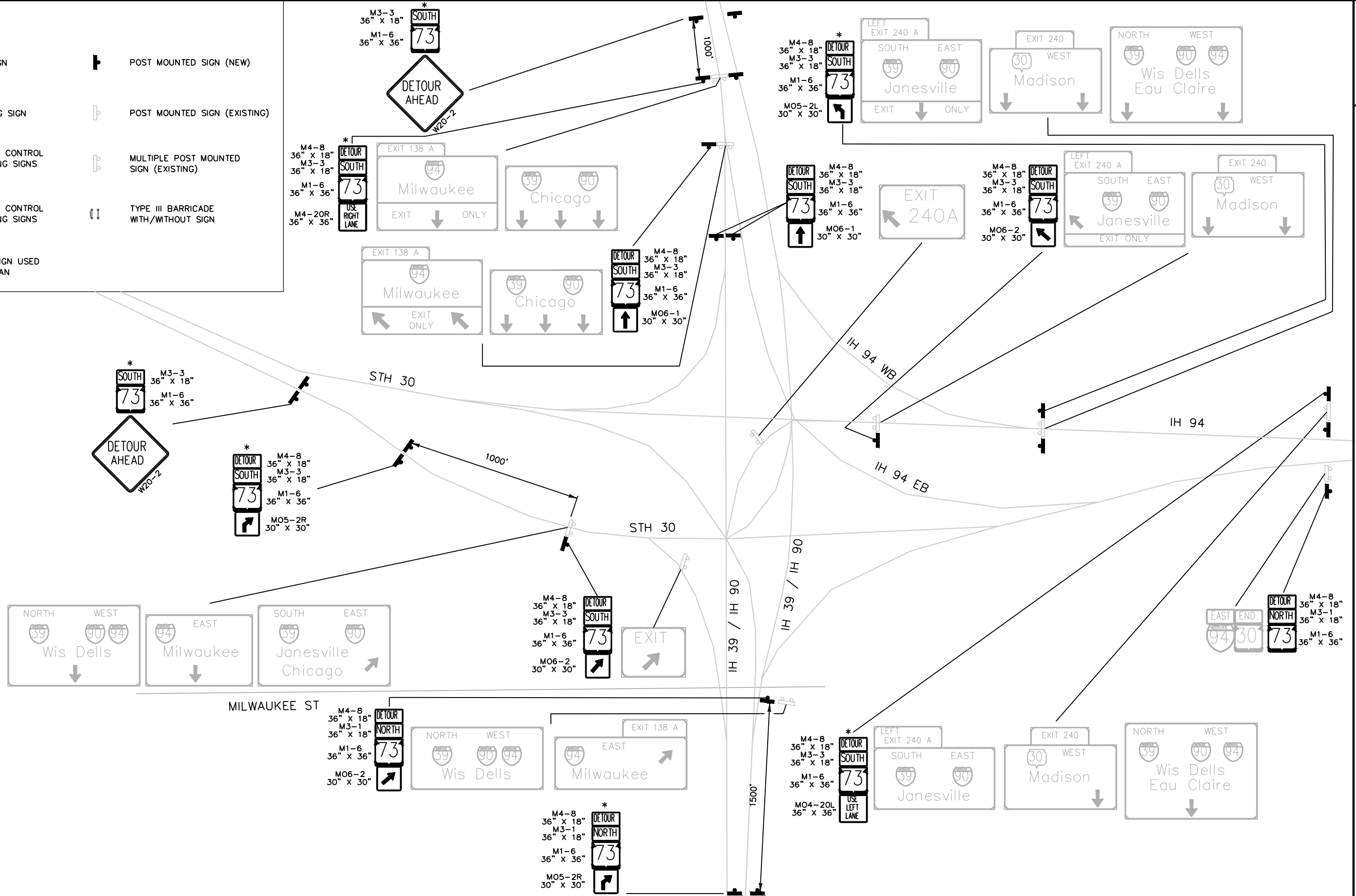
IH 39 / IH 90

IH 39 / IH 90



LEGEND

	NEW SIGN		POST MOUNTED SIGN (NEW)
	EXISTING SIGN		POST MOUNTED SIGN (EXISTING)
	TRAFFIC CONTROL COVERING SIGNS TYPE I		MULTIPLE POST MOUNTED SIGN (EXISTING)
	TRAFFIC CONTROL COVERING SIGNS TYPE II		TYPE III BARRICADE WITH/WITHOUT SIGN
* SAME SIGN USED IN MEDIAN			



PROJECT NO: 3070-04-60

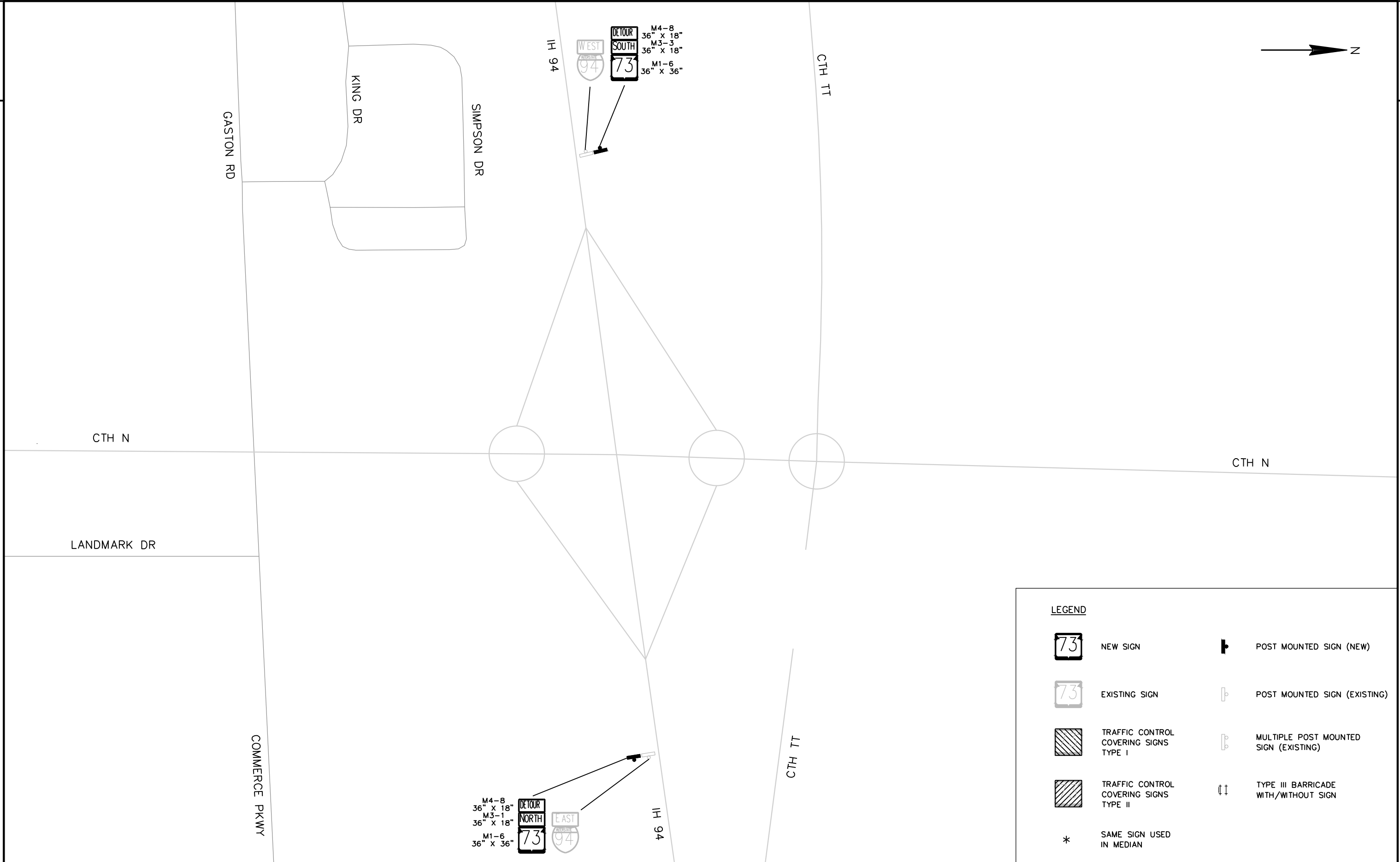
HWY: STH 73

COUNTY: DANE

DETOUR PLAN STH 73 - DETAIL VIEW 9

SHEET









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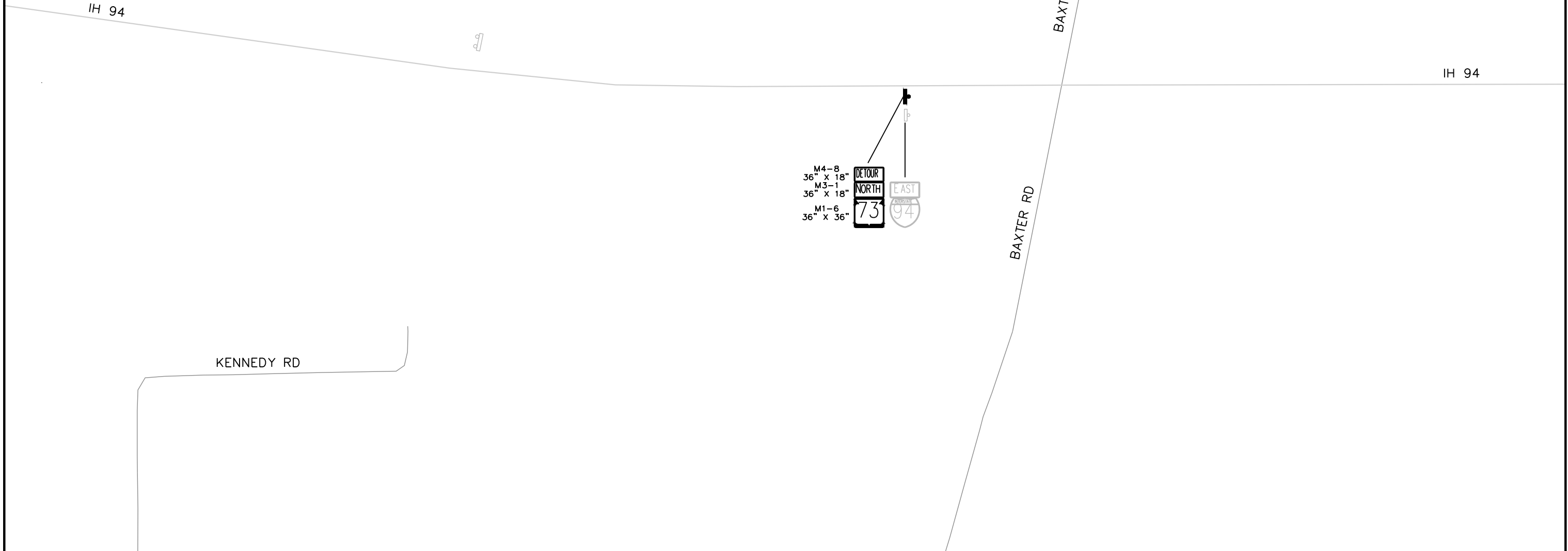


LEGEND

	NEW SIGN		POST MOUNTED SIGN (NEW)
	EXISTING SIGN		POST MOUNTED SIGN (EXISTING)
	TRAFFIC CONTROL COVERING SIGNS TYPE I		MULTIPLE POST MOUNTED SIGN (EXISTING)
	TRAFFIC CONTROL COVERING SIGNS TYPE II		TYPE III BARRICADE WITH/WITHOUT SIGN
*	SAME SIGN USED IN MEDIAN		

LEGEND

	NEW SIGN		POST MOUNTED SIGN (NEW)
	EXISTING SIGN		POST MOUNTED SIGN (EXISTING)
	TRAFFIC CONTROL COVERING SIGNS TYPE I		MULTIPLE POST MOUNTED SIGN (EXISTING)
	TRAFFIC CONTROL COVERING SIGNS TYPE II		TYPE III BARRICADE WITH/WITHOUT SIGN
*	SAME SIGN USED IN MEDIAN		



PROJECT NO: 3070-04-60	HWY: STH 73	COUNTY: DANE	DETOUR PLAN STH 73 - DETAIL VIEW 11	SHEET	E
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LEGEND



NEW SIGN



POST MOUNTED SIGN (NEW)



EXISTING SIGN



POST MOUNTED SIGN (EXISTING)



TRAFFIC CONTROL COVERING SIGNS TYPE I



MULTIPLE POST MOUNTED SIGN (EXISTING)



TRAFFIC CONTROL COVERING SIGNS TYPE II

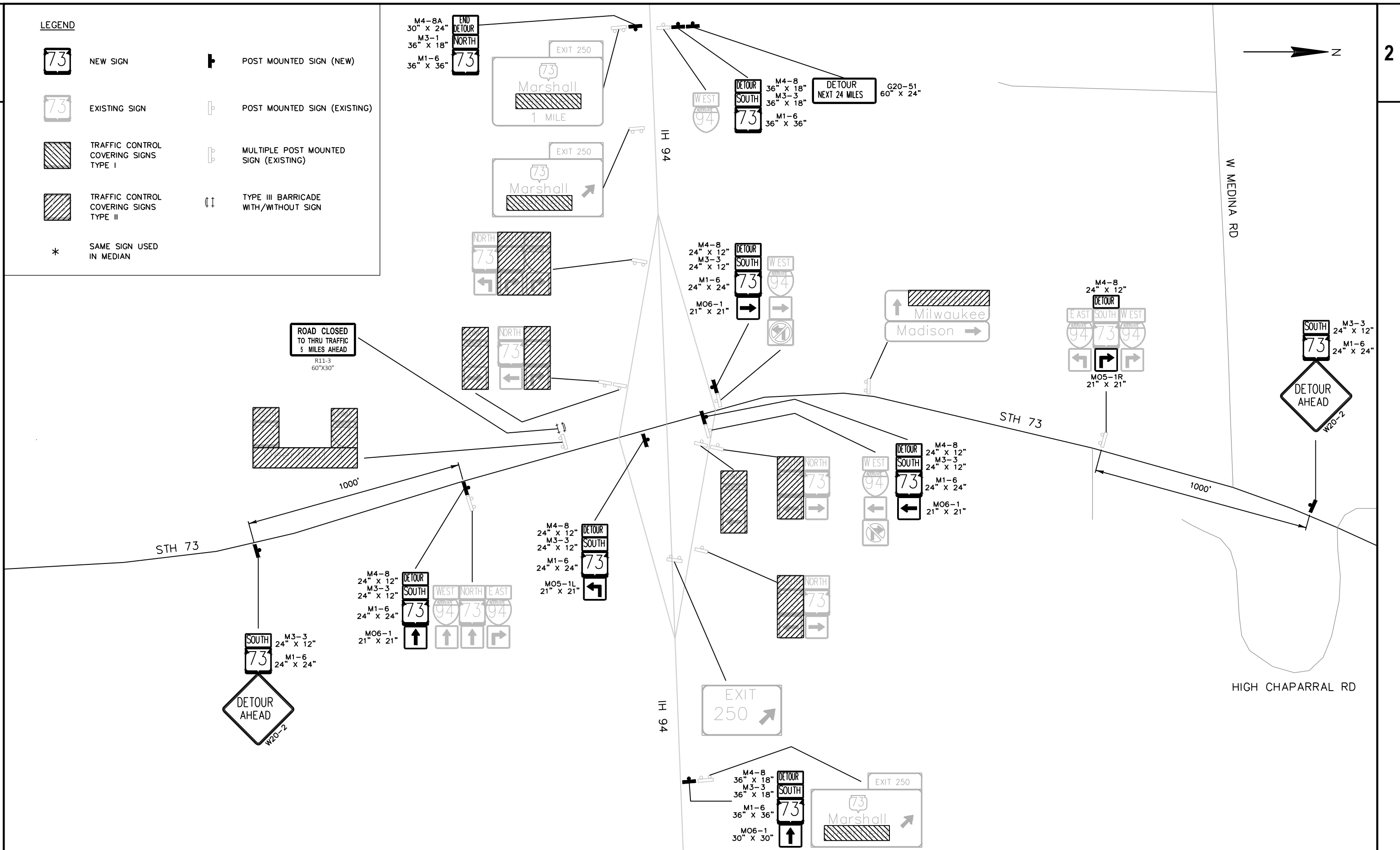


TYPE III BARRICADE WITH/WITHOUT SIGN

* SAME SIGN USED IN MEDIAN

2

2



PROJECT NO: 3070-04-60

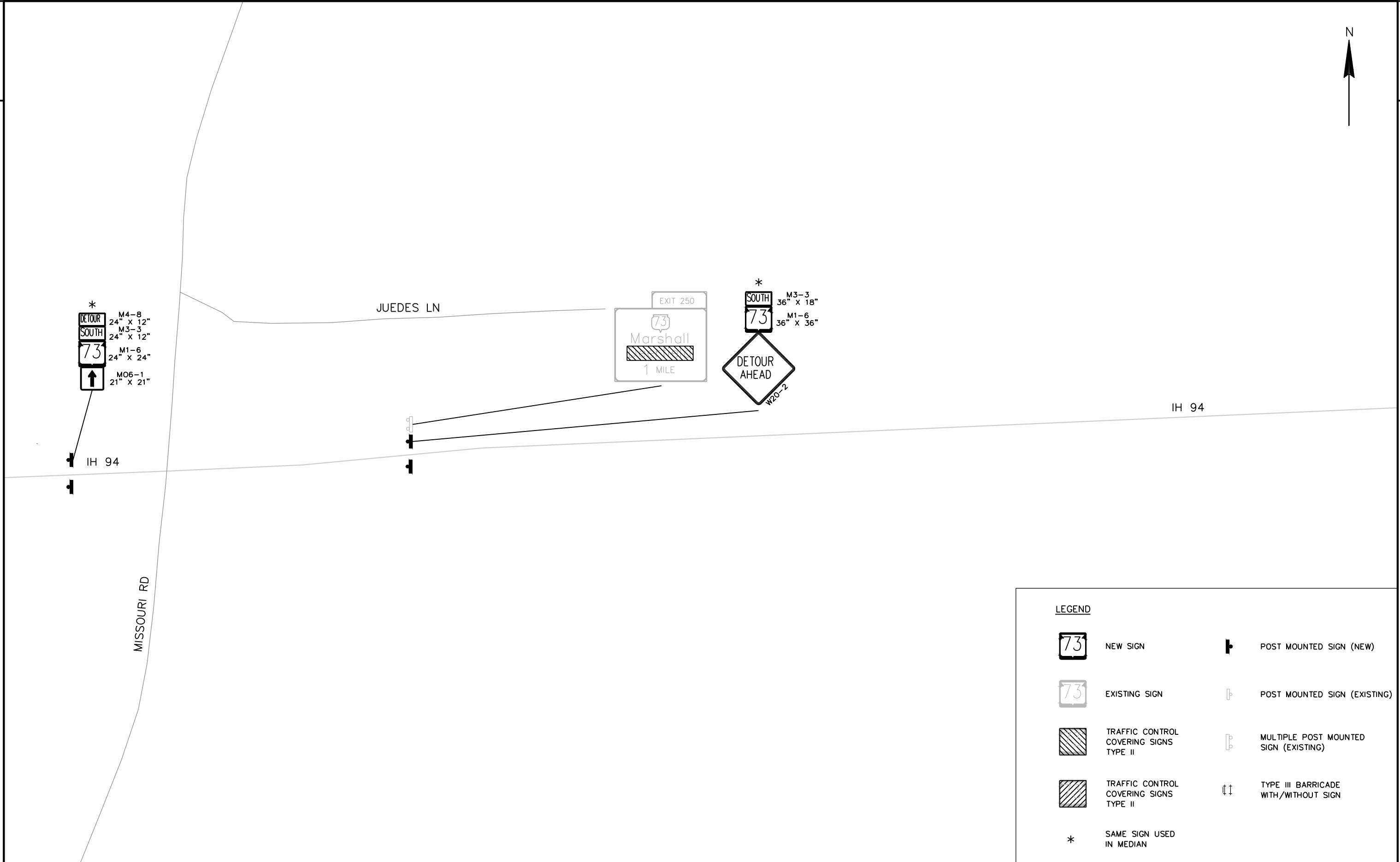
HWY: STH 73

COUNTY: DANE

DETOUR PLAN STH 73 - DETAIL VIEW 12

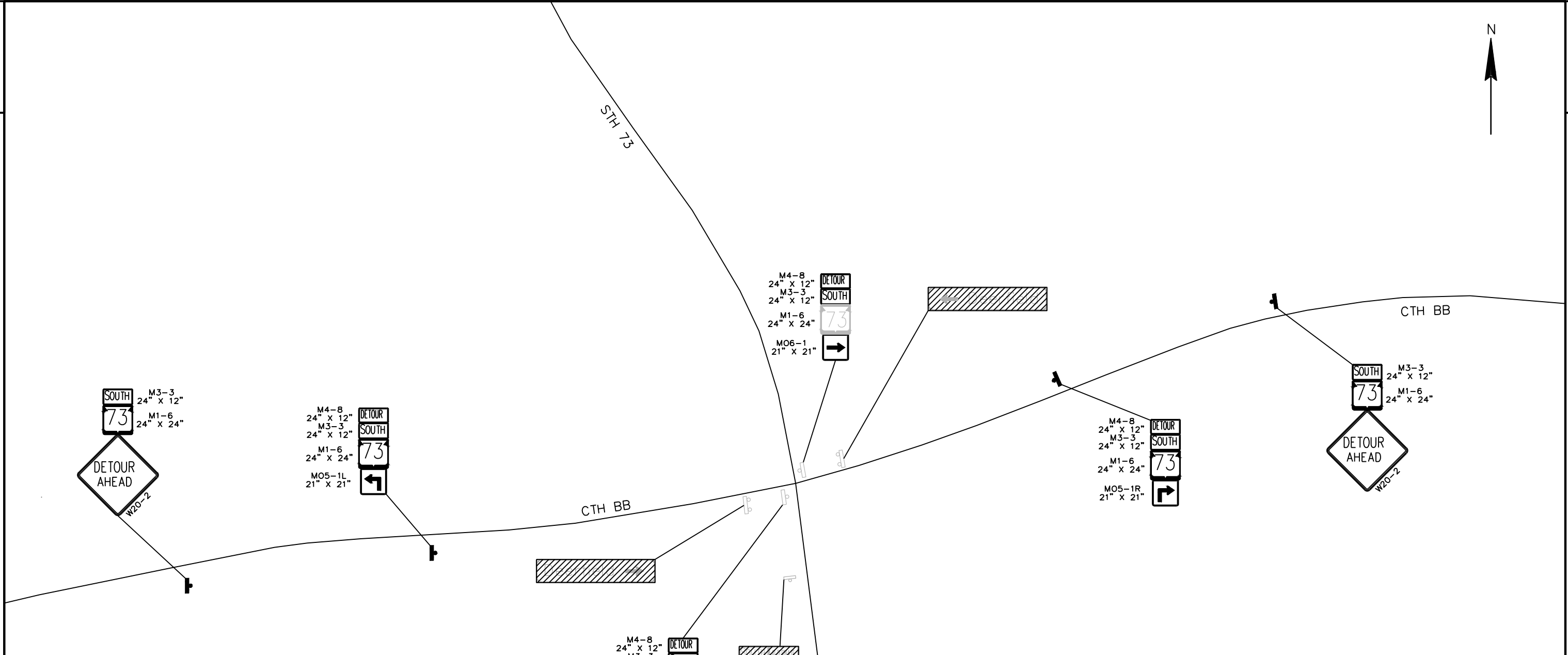
SHEET

E



LEGEND

	NEW SIGN		POST MOUNTED SIGN (NEW)
	EXISTING SIGN		POST MOUNTED SIGN (EXISTING)
	TRAFFIC CONTROL COVERING SIGNS TYPE II		MULTIPLE POST MOUNTED SIGN (EXISTING)
	TRAFFIC CONTROL COVERING SIGNS TYPE II		TYPE III BARRICADE WITH/WITHOUT SIGN
*	SAME SIGN USED IN MEDIAN		



LEGEND

	NEW SIGN		POST MOUNTED SIGN (NEW)
	EXISTING SIGN		POST MOUNTED SIGN (EXISTING)
	TRAFFIC CONTROL COVERING SIGNS TYPE I		MULTIPLE POST MOUNTED SIGN (EXISTING)
	TRAFFIC CONTROL COVERING SIGNS TYPE II		TYPE III BARRICADE WITH/WITHOUT SIGN

* SAME SIGN USED IN MEDIAN



LEGEND
 - - - BICYCLE DETOUR
 — PEDESTRIAN DETOUR

PROJECT NO: 3070-04-60

HWY: STH 73

COUNTY: DANE

TRAFFIC CONTROL - GLACIAL DRUMLIN STATE TRAIL DETOUR

SHEET

E

FILE NAME : Y:\50XX\5085_DP.STH73.DAN\CADD\30700460\025002-TC.DWG
 LAYOUT NAME - 025002-TC

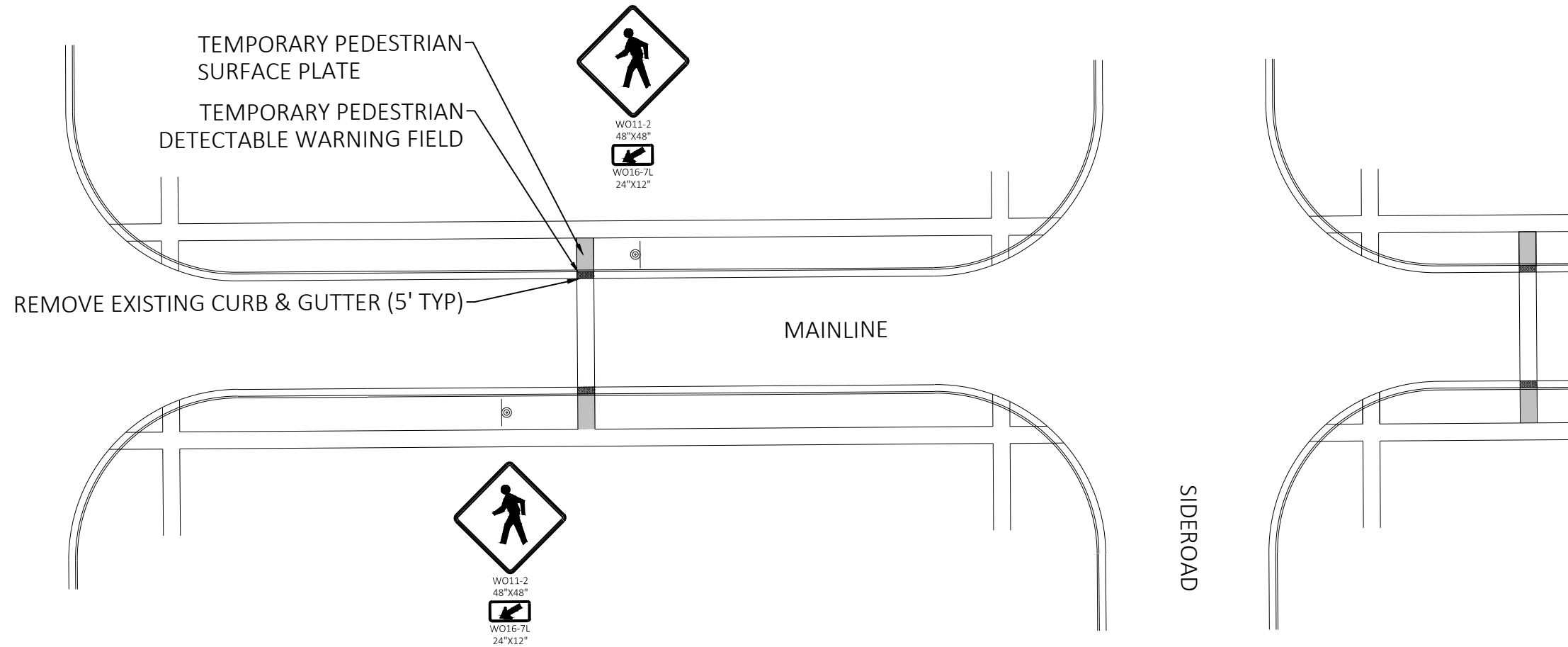
PLOT DATE : 7/27/2022 12:26 PM

PLOT BY : CORY INMAN

PLOT NAME :

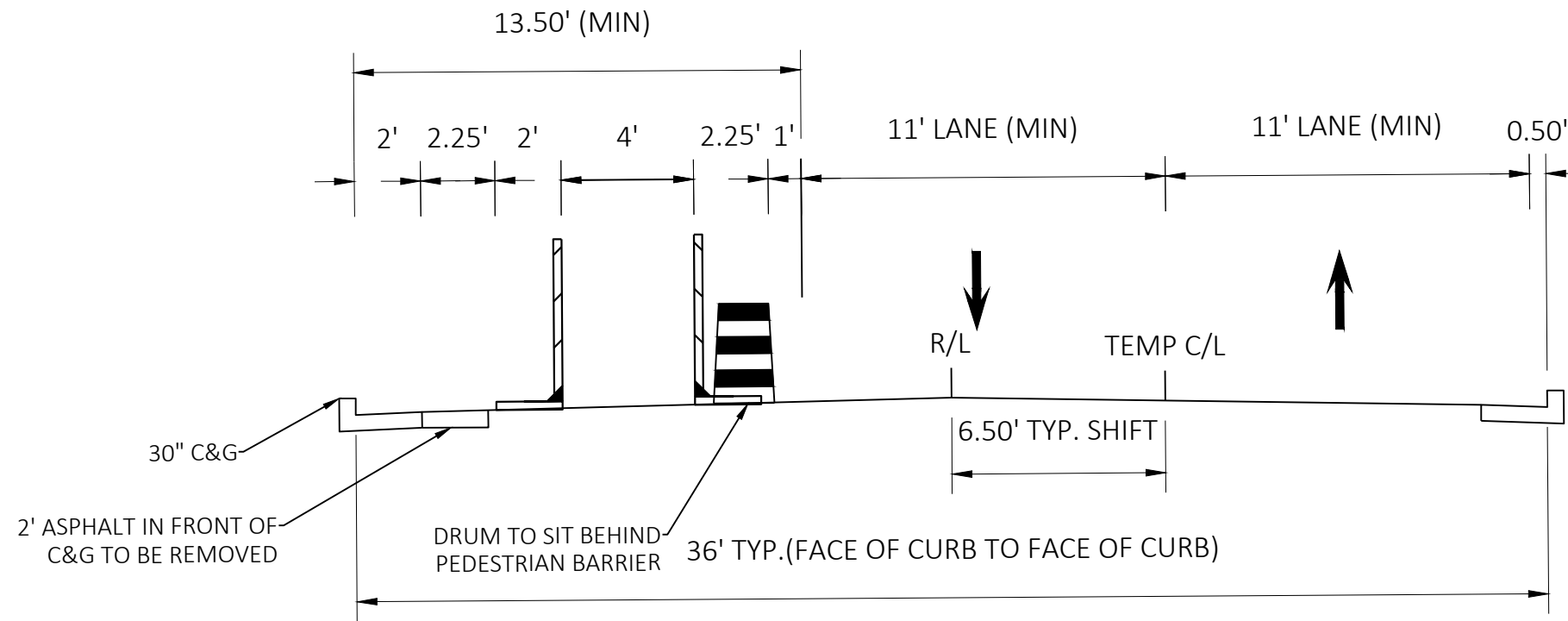
PLOT SCALE : 1 IN:200 FT

WISDOT/CADD SHEET 42

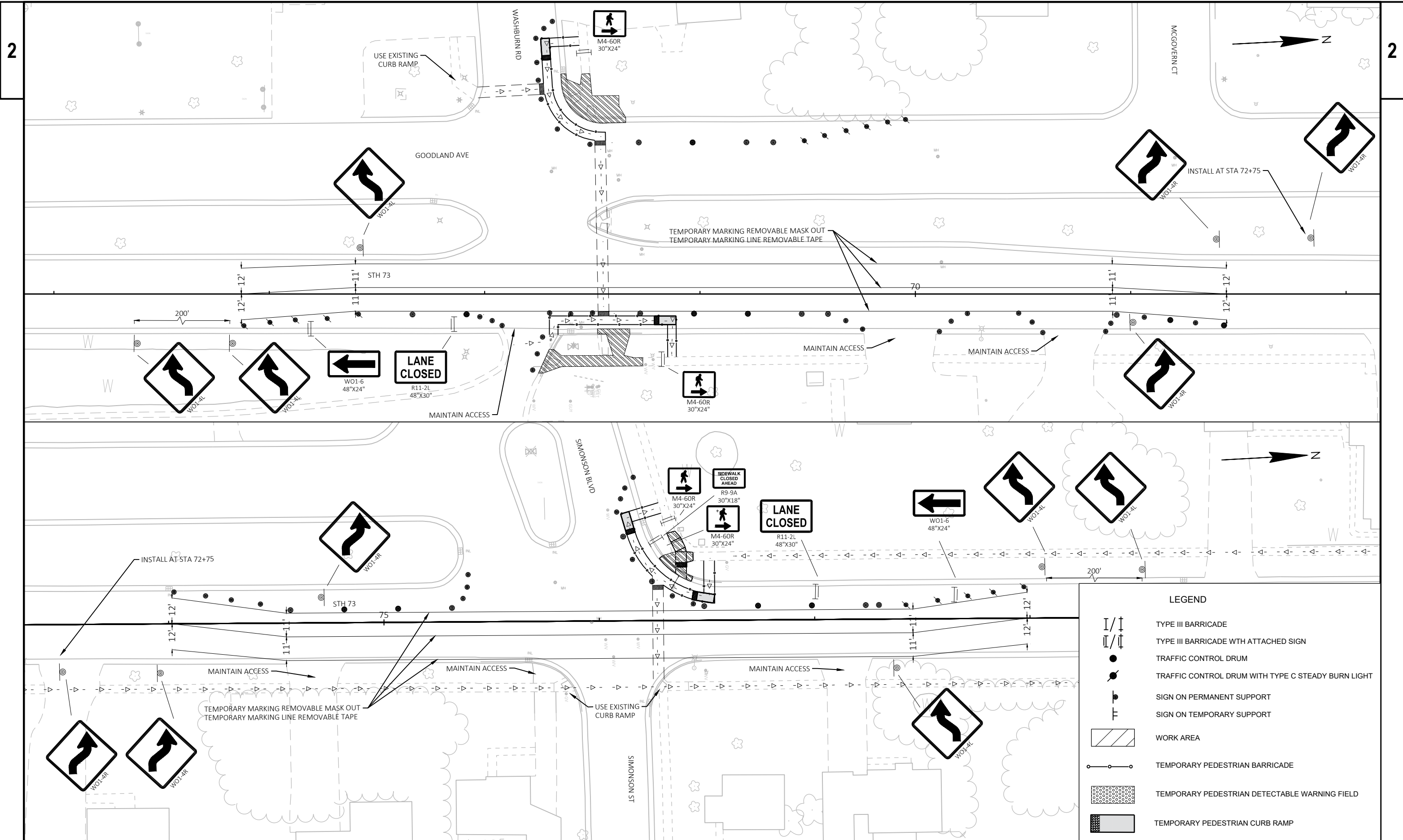


**TEMPORARY PEDESTRIAN ACCOMMODATION
MID-BLOCK CROSSING**

NOTE: DETAIL SHOW TYPICAL LAYOUT FOR MID BLOCK CROSSINGS DURING CONSTRUCTION, ADDITIONAL SIGNING AND DETAILS FOR ADJACENT TEMPORARY MID BLOCK CROSSINGS NOT SHOWN

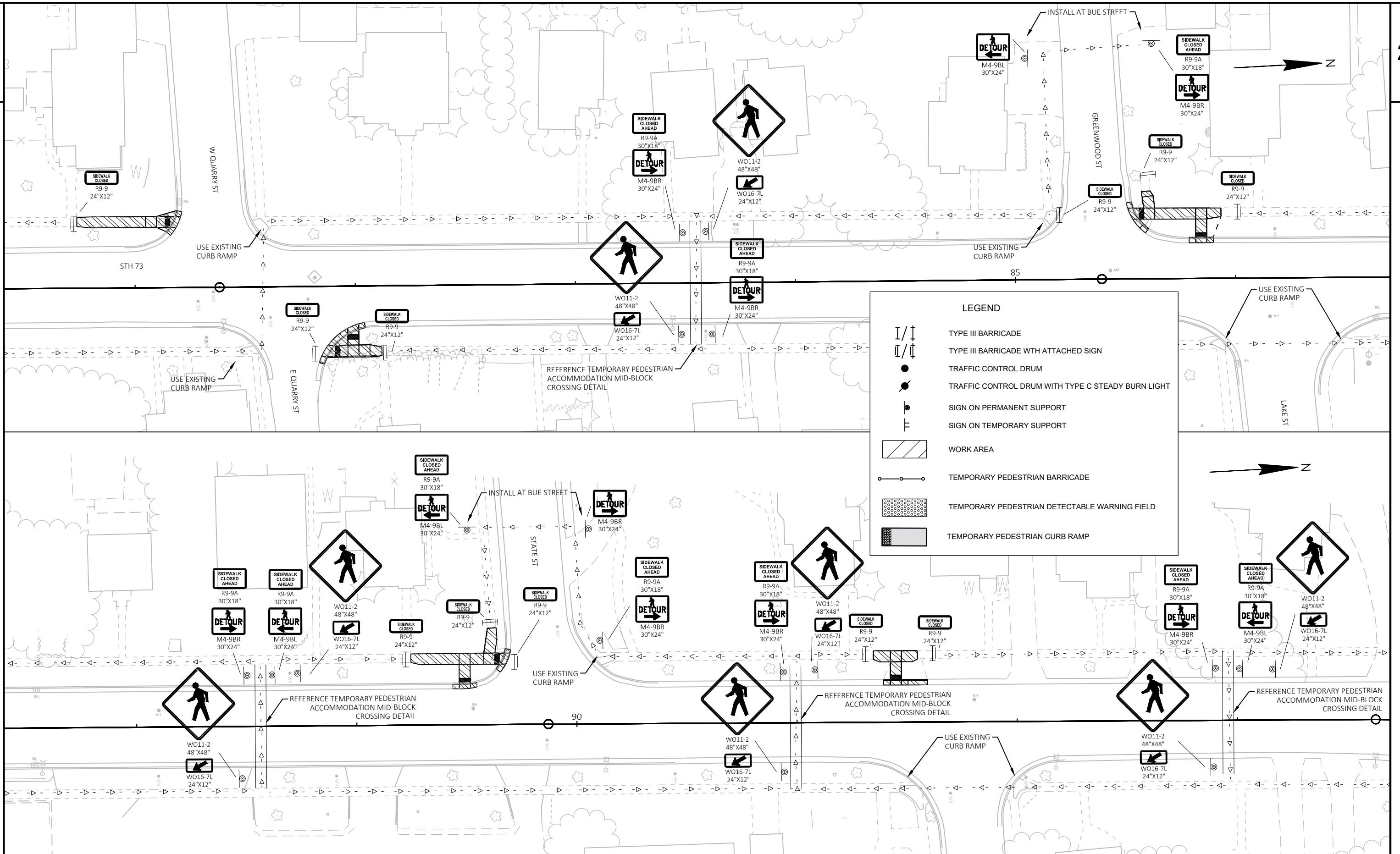


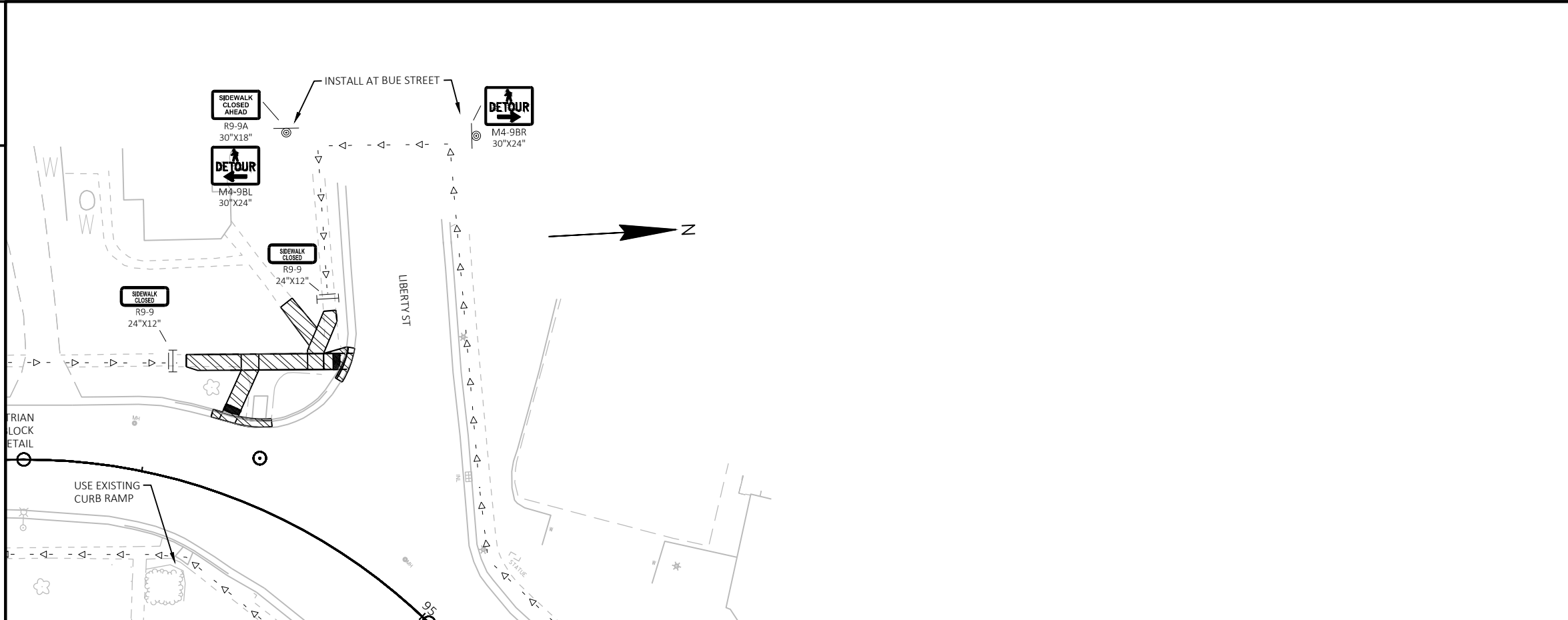
TEMPORARY SHOULDER CLOSURE & LANE SHIFT FOR CURB RAMP CONSTRUCTION



LEGEND	
	TYPE III BARRICADE
	TYPE III BARRICADE WITH ATTACHED SIGN
	TRAFFIC CONTROL DRUM
	TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
	SIGN ON PERMANENT SUPPORT
	SIGN ON TEMPORARY SUPPORT
	WORK AREA
	TEMPORARY PEDESTRIAN BARRICADE
	TEMPORARY PEDESTRIAN DETECTABLE WARNING FIELD
	TEMPORARY PEDESTRIAN CURB RAMP

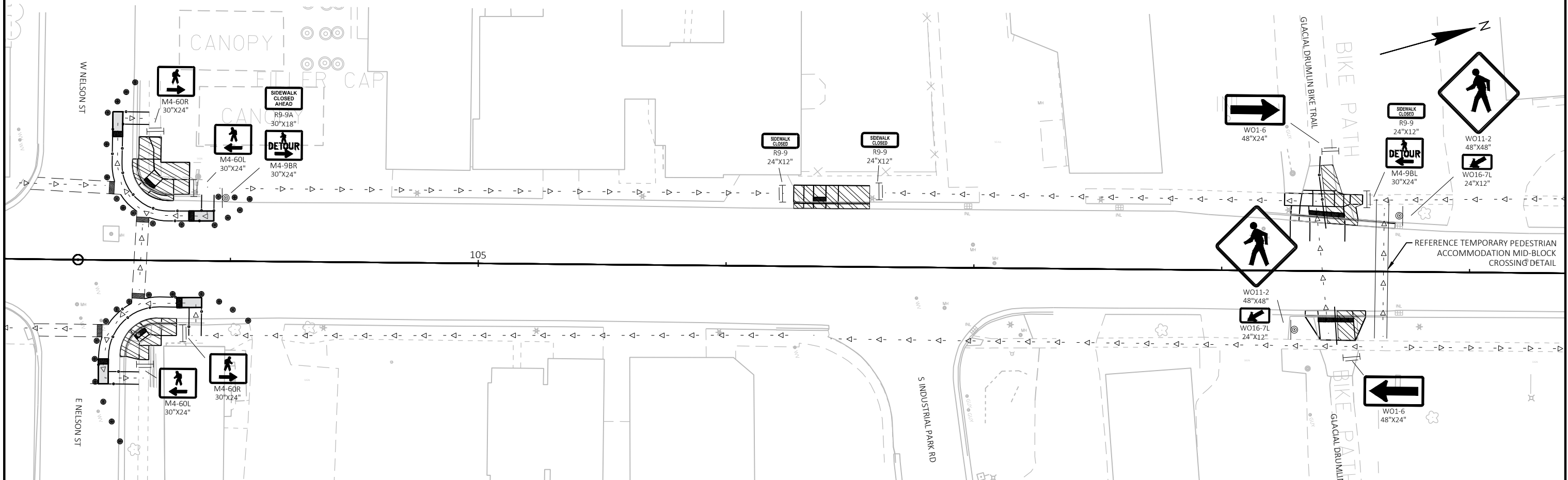
PROJECT NO: 3070-04-60 HWY: STH 73 COUNTY: DANE DETOUR PLAN - TEMPORARY PEDESTRIAN (STAGE 1) SHEET E

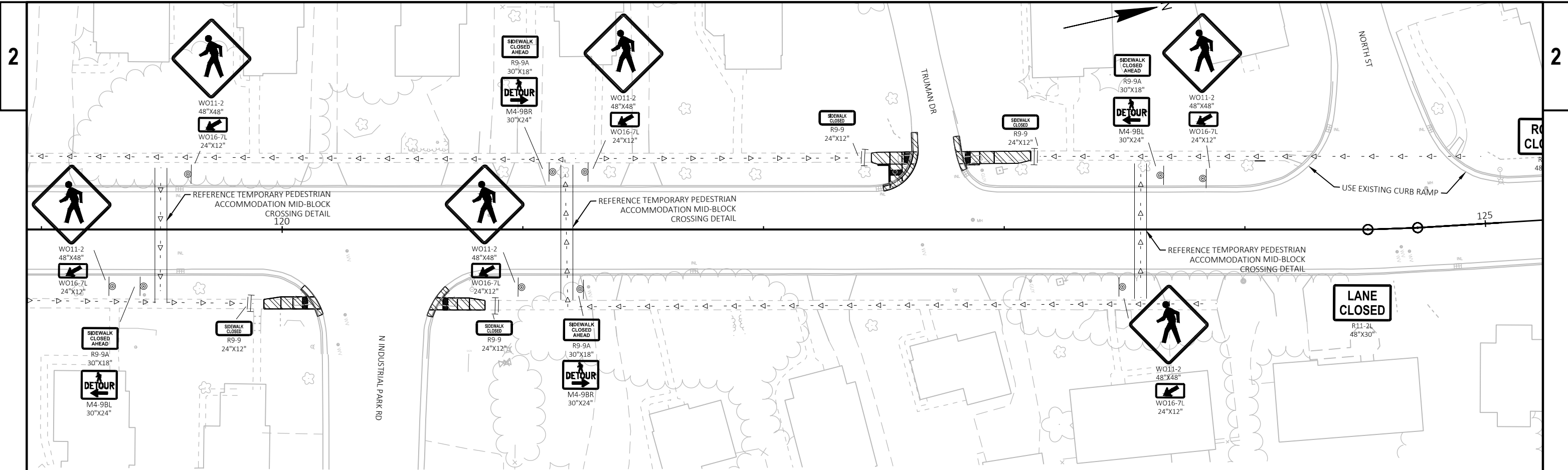




LEGEND

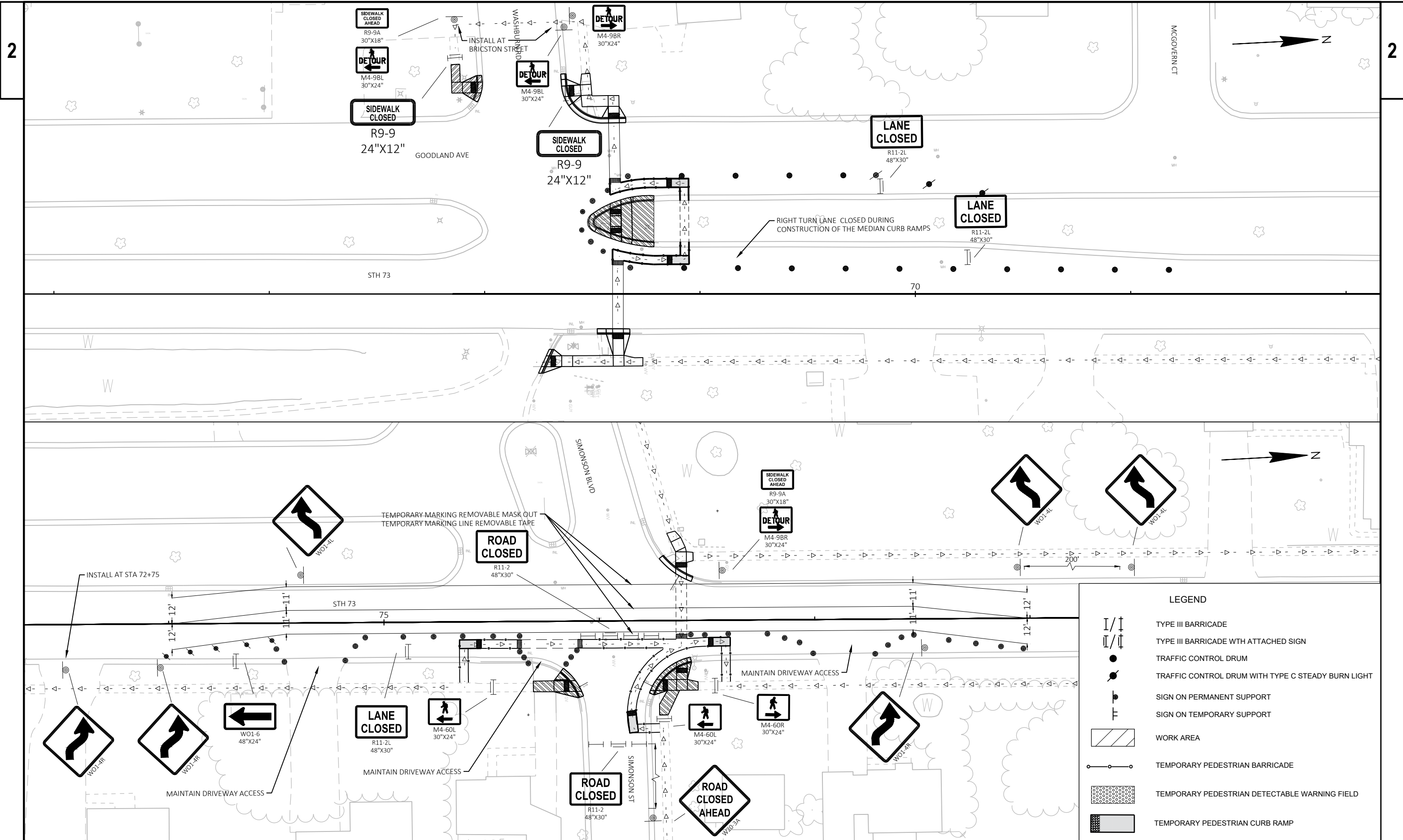
	TYPE III BARRICADE
	TYPE III BARRICADE WITH ATTACHED SIGN
	TRAFFIC CONTROL DRUM
	TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
	SIGN ON PERMANENT SUPPORT
	SIGN ON TEMPORARY SUPPORT
	WORK AREA
	TEMPORARY PEDESTRIAN BARRICADE
	TEMPORARY PEDESTRIAN DETECTABLE WARNING FIELD
	TEMPORARY PEDESTRIAN CURB RAMP





LEGEND	
	TYPE III BARRICADE
	TYPE III BARRICADE WTH ATTACHED SIGN
	TRAFFIC CONTROL DRUM
	TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
	SIGN ON PERMANENT SUPPORT
	SIGN ON TEMPORARY SUPPORT
	WORK AREA
	TEMPORARY PEDESTRIAN BARRICADE
	TEMPORARY PEDESTRIAN DETECTABLE WARNING FIELD
	TEMPORARY PEDESTRIAN CURB RAMP

PROJECT NO: 3070-04-60 HWY: STH 73 COUNTY: DANE DETOUR PLAN - TEMPORARY PEDESTRIAN (STAGE 1) SHEET E



PROJECT NO: 3070-04-60

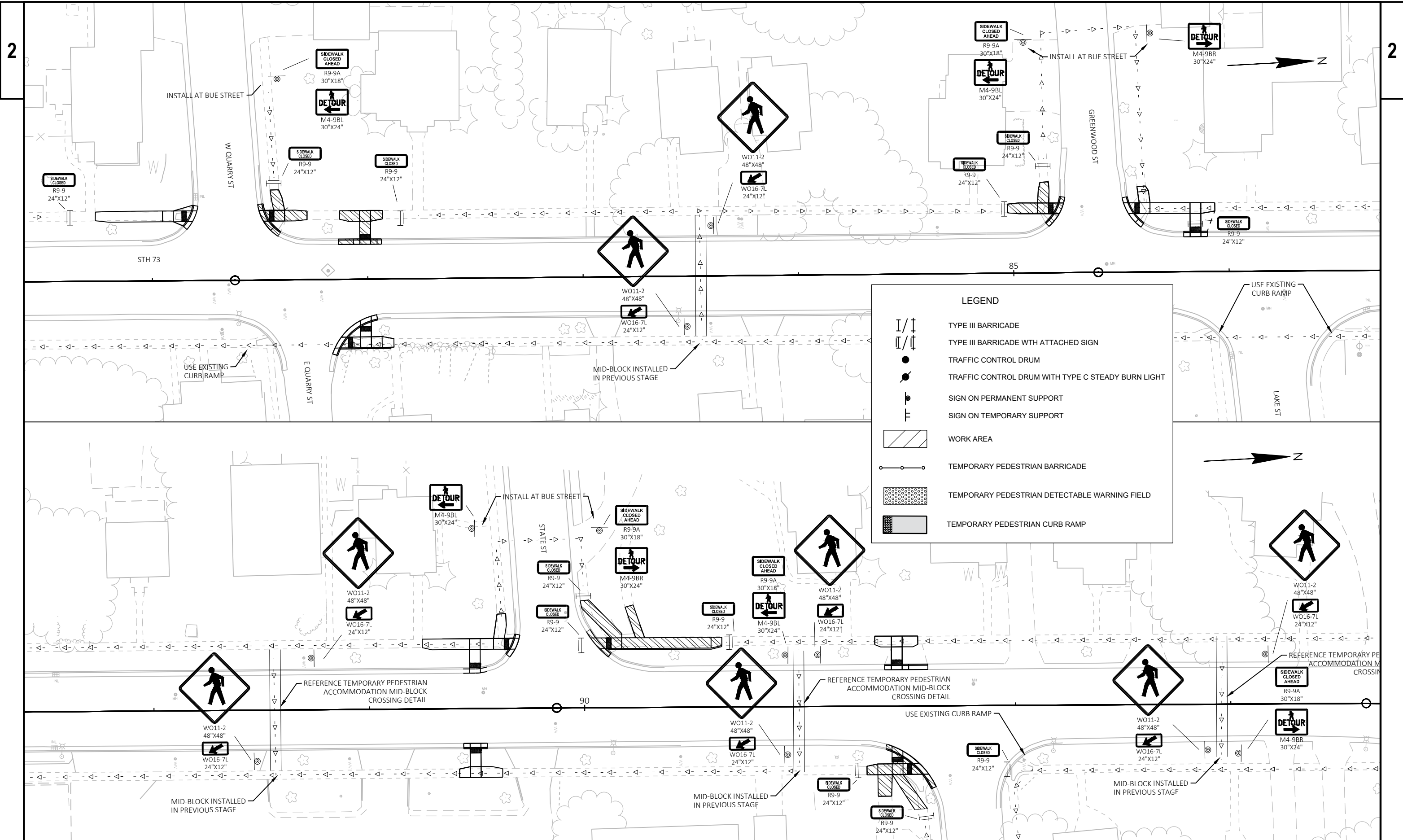
HWY: STH 73

COUNTY: DANE

DETOUR PLAN - TEMPORARY PEDESTRIAN (STAGE 2)

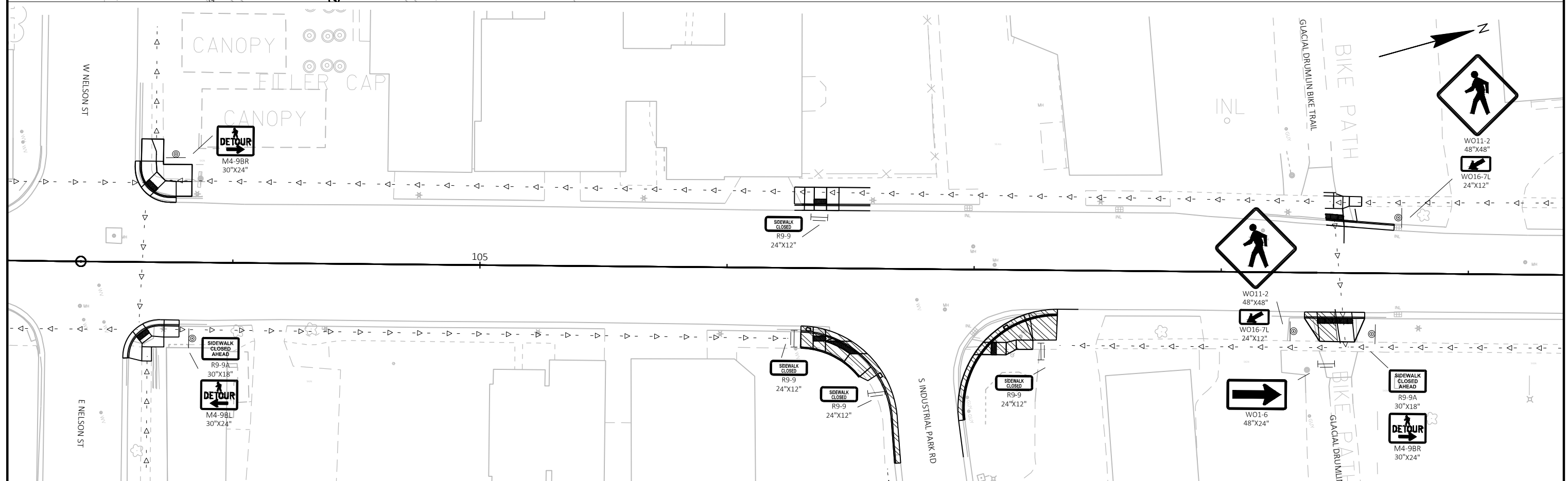
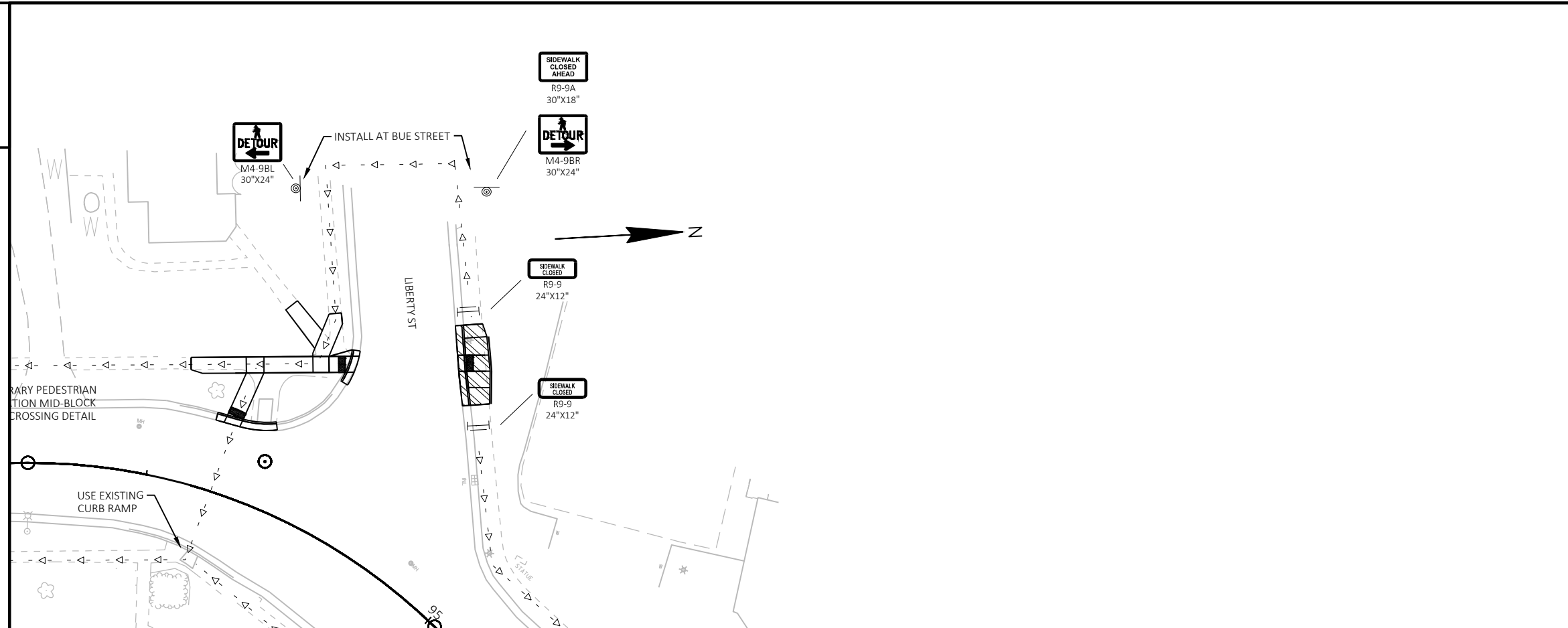
SHEET

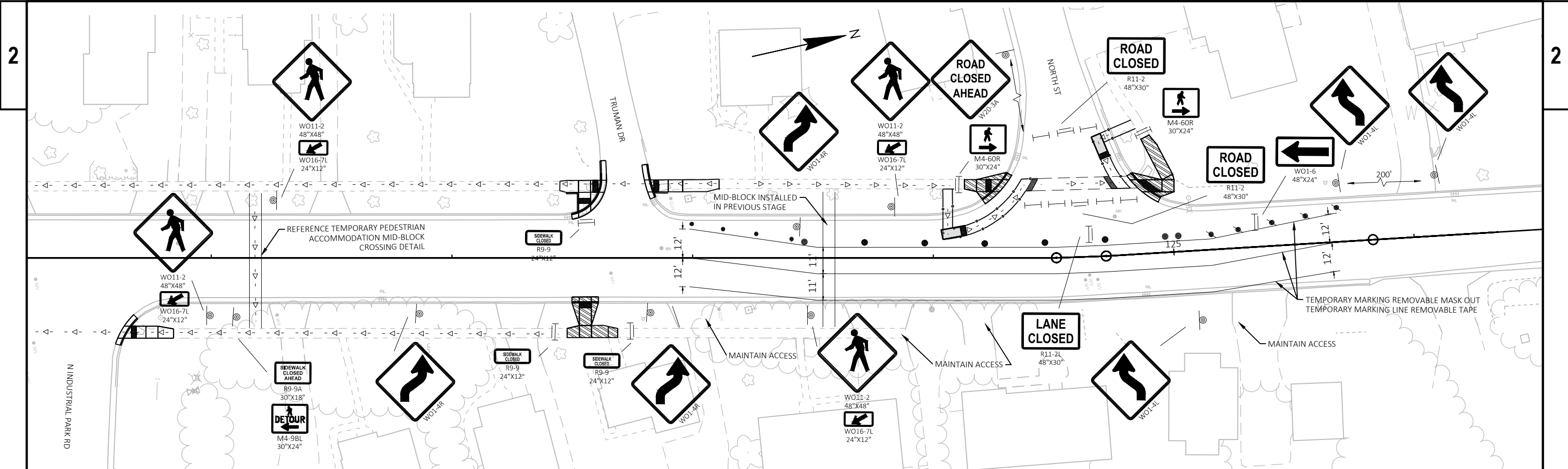
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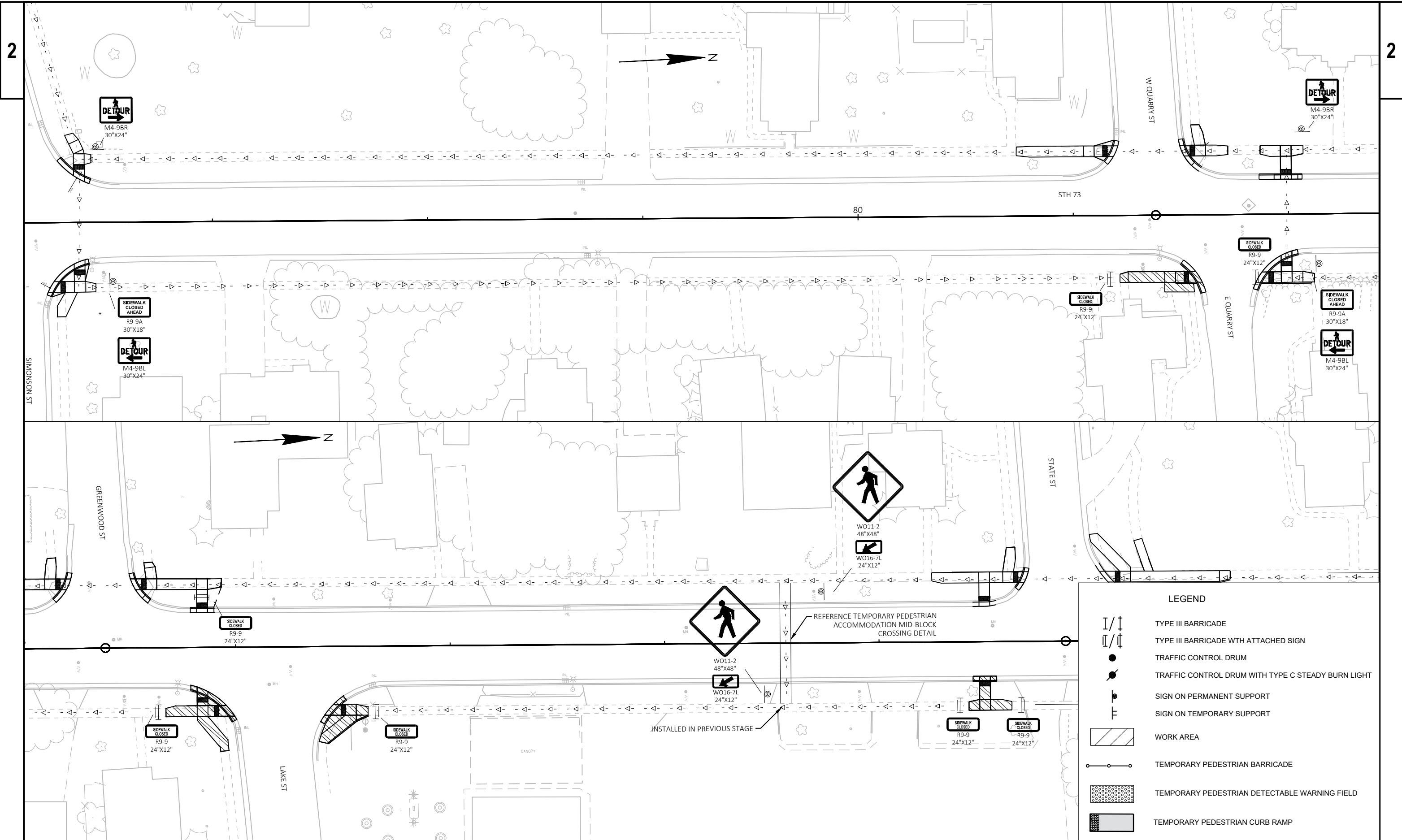
LEGEND

	TYPE III BARRICADE
	TYPE III BARRICADE WITH ATTACHED SIGN
	TRAFFIC CONTROL DRUM
	TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
	SIGN ON PERMANENT SUPPORT
	SIGN ON TEMPORARY SUPPORT
	WORK AREA
	TEMPORARY PEDESTRIAN BARRICADE
	TEMPORARY PEDESTRIAN DETECTABLE WARNING FIELD
	TEMPORARY PEDESTRIAN CURB RAMP





LEGEND	
	TYPE III BARRICADE
	TYPE III BARRICADE WITH ATTACHED SIGN
	TRAFFIC CONTROL DRUM
	TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
	SIGN ON PERMANENT SUPPORT
	SIGN ON TEMPORARY SUPPORT
	WORK AREA
	TEMPORARY PEDESTRIAN BARRICADE
	TEMPORARY PEDESTRIAN DETECTABLE WARNING FIELD
	TEMPORARY PEDESTRIAN CURB RAMP



PROJECT NO: 3070-04-60

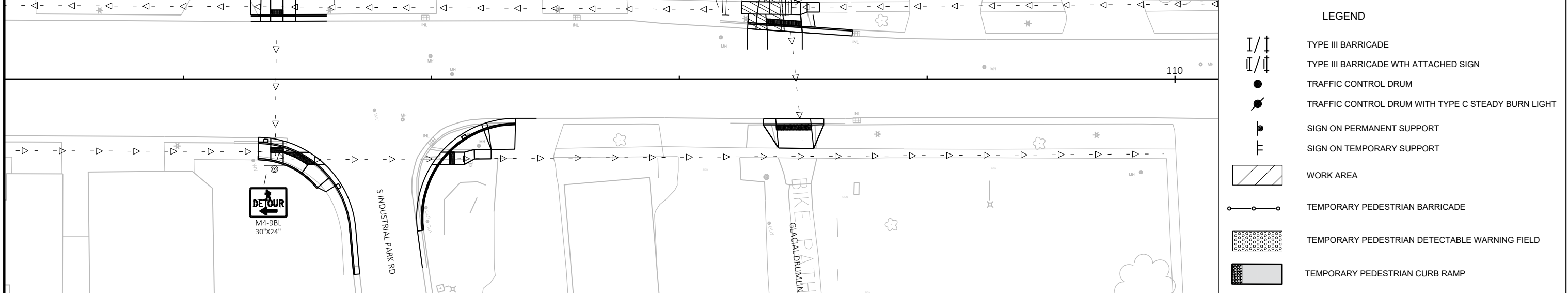
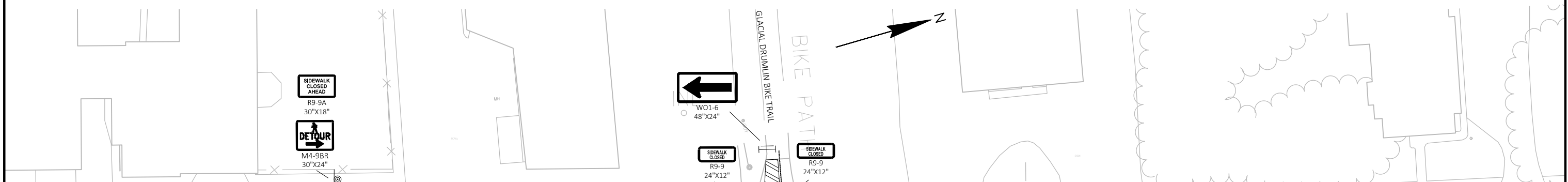
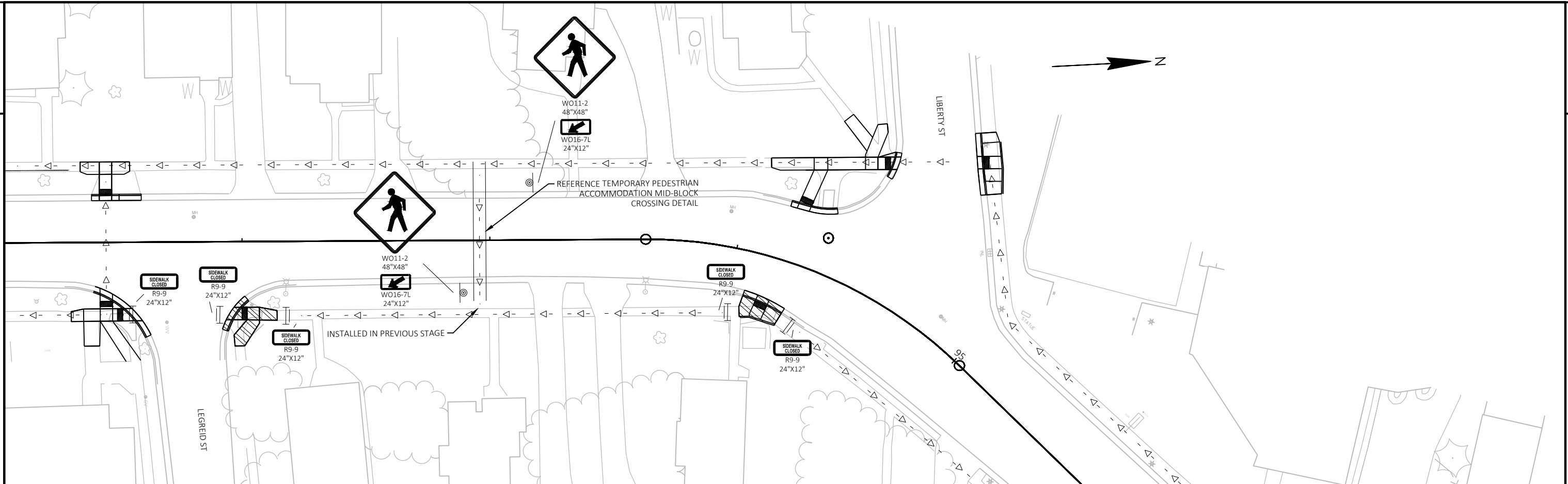
HWY: STH 73

COUNTY: DANE

DETOUR PLAN - TEMPORARY PEDESTRIAN (STAGE 3)

SHEET

E



LEGEND

	TYPE III BARRICADE
	TYPE III BARRICADE WTH ATTACHED SIGN
	TRAFFIC CONTROL DRUM
	TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
	SIGN ON PERMANENT SUPPORT
	SIGN ON TEMPORARY SUPPORT
	WORK AREA
	TEMPORARY PEDESTRIAN BARRICADE
	TEMPORARY PEDESTRIAN DETECTABLE WARNING FIELD
	TEMPORARY PEDESTRIAN CURB RAMP

Estimate Of Quantities

3070-04-60

Line	Item	Item Description	Unit	Total	Qty
0002	204.0100	Removing Concrete Pavement	SY	18.000	18.000
0004	204.0110	Removing Asphaltic Surface	SY	235.000	235.000
0006	204.0115	Removing Asphaltic Surface Butt Joints	SY	841.000	841.000
0008	204.0120	Removing Asphaltic Surface Milling	SY	31,332.000	31,332.000
0010	204.0150	Removing Curb & Gutter	LF	997.000	997.000
0012	204.0155	Removing Concrete Sidewalk	SY	681.000	681.000
0014	204.0220	Removing Inlets	EACH	1.000	1.000
0016	204.0245	Removing Storm Sewer (size) 01. 15-Inch	LF	12.000	12.000
0018	204.0245	Removing Storm Sewer (size) 01. 2-Inch	LF	12.000	12.000
0020	205.0100	Excavation Common	CY	5,881.000	5,881.000
0022	211.0101	Prepare Foundation for Asphaltic Paving (project) 01. 3070-04-60	EACH	1.000	1.000
0024	213.0100	Finishing Roadway (project) 01. 3070-04-60	EACH	1.000	1.000
0026	305.0110	Base Aggregate Dense 3/4-Inch	TON	489.000	489.000
0028	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	4,676.000	4,676.000
0030	305.0130	Base Aggregate Dense 3-Inch	TON	4,676.000	4,676.000
0032	416.0160	Concrete Driveway 6-Inch	SY	12.000	12.000
0034	416.0260	Concrete Driveway HES 6-Inch	SY	22.000	22.000
0036	455.0605	Tack Coat	GAL	5,314.000	5,314.000
0038	460.2000	Incentive Density HMA Pavement	DOL	5,460.000	5,460.000
0040	460.6223	HMA Pavement 3 MT 58-28 S	TON	4,782.000	4,782.000
0042	460.6224	HMA Pavement 4 MT 58-28 S	TON	3,720.000	3,720.000
0044	465.0105	Asphaltic Surface	TON	795.000	795.000
0046	465.0110	Asphaltic Surface Patching	TON	67.000	67.000
0048	465.0125	Asphaltic Surface Temporary	TON	5.000	5.000
0050	465.0475	Asphalt Centerline Rumble Strips 2-Lane Rural	LF	2,832.000	2,832.000
0052	601.0411	Concrete Curb & Gutter 30-Inch Type D	LF	1,051.000	1,051.000
0054	601.0600	Concrete Curb Pedestrian	LF	199.000	199.000
0056	602.0405	Concrete Sidewalk 4-Inch	SF	7,205.000	7,205.000
0058	602.0415	Concrete Sidewalk 6-Inch	SF	306.000	306.000
0060	602.0505	Curb Ramp Detectable Warning Field Yellow	SF	620.000	620.000
0062	602.0605	Curb Ramp Detectable Warning Field Radial Yellow	SF	36.000	36.000
0064	608.0315	Storm Sewer Pipe Reinforced Concrete Class III 15-Inch	LF	90.000	90.000
0066	611.0624	Inlet Covers Type H	EACH	2.000	2.000
0068	611.3004	Inlets 4-FT Diameter	EACH	3.000	3.000
0070	611.8110	Adjusting Manhole Covers	EACH	11.000	11.000
0072	611.9710	Salvaged Inlet Covers	EACH	1.000	1.000
0074	614.0400	Adjusting Steel Plate Beam Guard	LF	800.000	800.000
0076	614.0950	Replacing Guardrail Posts and Blocks	EACH	20.000	20.000
0078	614.0951	Replacing Guardrail Rail and Hardware	LF	100.000	100.000
0080	614.0952	Replacing Guardrail Reflectors	EACH	8.000	8.000
0082	618.0100	Maintenance And Repair of Haul Roads (project) 01. 3070-04-60	EACH	1.000	1.000
0084	619.1000	Mobilization	EACH	1.000	1.000
0086	620.0300	Concrete Median Sloped Nose	SF	32.000	32.000
0088	624.0100	Water	MGAL	94.000	94.000
0090	625.0105	Topsoil	CY	28.000	28.000
0092	628.1905	Mobilizations Erosion Control	EACH	5.000	5.000
0094	628.1910	Mobilizations Emergency Erosion Control	EACH	5.000	5.000
0096	628.7005	Inlet Protection Type A	EACH	3.000	3.000
0098	628.7015	Inlet Protection Type C	EACH	37.000	37.000

Estimate Of Quantities

3070-04-60

Line	Item	Item Description	Unit	Total	Qty
0100	631.0300	Sod Water	MGAL	9.300	9.300
0102	631.1000	Sod Lawn	SY	485.000	485.000
0104	634.0614	Posts Wood 4x6-Inch X 14-FT	EACH	9.000	9.000
0106	637.2210	Signs Type II Reflective H	SF	39.000	39.000
0108	637.2230	Signs Type II Reflective F	SF	4.500	4.500
0110	638.2102	Moving Signs Type II	EACH	4.000	4.000
0112	638.3000	Removing Small Sign Supports	EACH	4.000	4.000
0114	638.4000	Moving Small Sign Supports	EACH	1.000	1.000
0116	642.5201	Field Office Type C	EACH	1.000	1.000
0118	643.0300	Traffic Control Drums	DAY	462.000	462.000
0120	643.0420	Traffic Control Barricades Type III	DAY	941.000	941.000
0122	643.0705	Traffic Control Warning Lights Type A	DAY	449.000	449.000
0124	643.0715	Traffic Control Warning Lights Type C	DAY	83.000	83.000
0126	643.0900	Traffic Control Signs	DAY	8,511.000	8,511.000
0128	643.0910	Traffic Control Covering Signs Type I	EACH	4.000	4.000
0130	643.0920	Traffic Control Covering Signs Type II	EACH	74.000	74.000
0132	643.1050	Traffic Control Signs PCMS	DAY	28.000	28.000
0134	643.3120	Temporary Marking Line Epoxy 4-Inch	LF	15,848.000	15,848.000
0136	643.3150	Temporary Marking Line Removable Tape 4-Inch	LF	4,560.000	4,560.000
0138	643.3960	Temporary Marking Removable Mask Out Tape 6-Inch	LF	1,280.000	1,280.000
0140	643.3970	Temporary Marking Removable Mask Out Tape 10-Inch	LF	170.000	170.000
0142	643.5000	Traffic Control	EACH	1.000	1.000
0144	644.1430	Temporary Pedestrian Surface Plate	SF	2,305.000	2,305.000
0146	644.1601	Temporary Pedestrian Curb Ramp	DAY	377.000	377.000
0148	644.1605	Temporary Pedestrian Detectable Warning Field	SF	308.000	308.000
0150	644.1810	Temporary Pedestrian Barricade	LF	1,696.000	1,696.000
0152	646.1040	Marking Line Grooved Wet Ref Epoxy 4-Inch	LF	6,125.000	6,125.000
0154	646.3040	Marking Line Grooved Wet Ref Epoxy 8-Inch	LF	160.000	160.000
0156	646.4520	Marking Line Same Day Epoxy 4-Inch	LF	15,848.000	15,848.000
0158	646.5020	Marking Arrow Epoxy	EACH	7.000	7.000
0160	646.5220	Marking Symbol Epoxy	EACH	8.000	8.000
0162	646.6120	Marking Stop Line Epoxy 18-Inch	LF	335.000	335.000
0164	646.7420	Marking Crosswalk Epoxy Transverse Line 6-Inch	LF	2,097.000	2,097.000
0166	646.8120	Marking Curb Epoxy	LF	45.000	45.000
0168	646.8220	Marking Island Nose Epoxy	EACH	1.000	1.000
0170	646.8320	Marking Parking Stall Epoxy	LF	685.000	685.000
0172	650.4000	Construction Staking Storm Sewer	EACH	3.000	3.000
0174	650.4500	Construction Staking Subgrade	LF	1,900.000	1,900.000
0176	650.5000	Construction Staking Base	LF	1,900.000	1,900.000
0178	650.8000	Construction Staking Resurfacing Reference	LF	9,300.000	9,300.000
0180	650.9000	Construction Staking Curb Ramps	EACH	42.000	42.000
0182	650.9500	Construction Staking Sidewalk (project) 01. 3070-04-60	EACH	1.000	1.000
0184	650.9911	Construction Staking Supplemental Control (project) 01. 3070-04-60	EACH	1.000	1.000
0186	690.0150	Sawing Asphalt	LF	1,953.000	1,953.000
0188	690.0250	Sawing Concrete	LF	476.000	476.000
0190	740.0440	Incentive IRI Ride	DOL	3,500.000	3,500.000
0192	ASP.1T0A	On-the-Job Training Apprentice at \$5.00/HR	HRS	700.000	700.000
0194	ASP.1T0G	On-the-Job Training Graduate at \$5.00/HR	HRS	350.000	350.000
0196	SPV.0060	Special 01. Management of Contaminated Groundwater	EACH	1.000	1.000

Estimate Of Quantities

3070-04-60

Line	Item	Item Description	Unit	Total	Qty
0198	SPV.0060	Special 02. Research and Locate Existing Land Parcel Monuments	EACH	157.000	157.000
0200	SPV.0060	Special 03. Verify and Replace Existing Land Parcel Monuments	EACH	157.000	157.000
0202	SPV.0060	Special 04. Adjusting Water Valve Boxes	EACH	12.000	12.000
0204	SPV.0060	Special 05. Adjusting Sanitary Manhole Covers	EACH	12.000	12.000
0206	SPV.0090	Special 01. Profile Curb Cut	LF	105.000	105.000
0208	SPV.0090	Special 02. Storm Sewer Pipe PVC 2-Inch	LF	25.000	25.000
0210	SPV.0195	Special 01. Excavation, Hauling, Disposal Petroleum Contaminated Soil (Direct Landfill)	TON	5.000	5.000

REMOVAL ITEMS (ASPHALT)

204.0120 204.0115

REMOVING ASPHALTIC SURFACE MILLING
REMOVING ASPHALTIC SURFACE BUTT JOINTS

CATEGORY	STATION	TO	STATION	OFFSET	ALIGNMENT	SY	SY	COMMENTS
0010	32+74	-	61+06	RT/LT	STH 73	9,440	20	STH 73 MAINLINE
	61+06	-	81+60	RT/LT	STH 73	7,304	46	STH 73 MAINLINE
	87+60	-	95+00	RT/LT	STH 73	2,632	18	STH 73 MAINLINE
	95+00	-	106+30	RT/LT	STH 73	4,676	26	STH 73 MAINLINE
	113+00	-	119+50	RT/LT	STH 73	2,312	36	STH 73 MAINLINE
	52+04	-	61+06	LT	STH 73	791	--	PASSING LANE
	52+83	-	57+79	RT	STH 73	593	19	LONDON RD
	67+77	-	71+25	LT	STH 73	320	44	WASHBURN RD, TURN LANE
	75+23	-	76+57	LT	STH 73	206	47	SIMONSON BLVD
	75+66	-	76+47	RT	STH 73	120	15	SIMONSON BLVD
	80+99	-	81+72	LT	STH 73	103	15	QUARRY ST
	81+34	-	82+07	RT	STH 73	92	11	QUARRY ST
	85+04	-	85+74	LT	STH 73	104	48	GREENWOOD ST
	85+74	-	86+65	RT	STH 73	131	71	LAKE ST
	89+49	-	90+31	LT	STH 73	115	15	STATE ST
	91+32	-	92+15	RT	STH 73	138	14	LEGREID ST
	93+77	-	95+19	LT	STH 73	425	16	LIBERTY ST
	98+02	-	98+56	LT	STH 73	53	16	DEERFIELD ST
	98+02	-	98+56	RT	STH 73	63	17	DEERFIELD ST
	103+18	-	10+37	RT	STH 73	79	21	NELSON ST
	103+18	-	10+37	RT	STH 73	61	18	NELSON ST
	106+29	-	107+00	RT	STH 73	184	53	S. INDUSTRIAL PARK RD
	119+94	-	120+85	RT	STH 73	224	71	N. INDUSTRIAL PARK RD
	122+50	-	122+94	LT	STH 73	54	31	TRUMAN DR
	124+02	-	125+00	LT	STH 73	188	72	NORTH ST
							81	MANHOLES
SUBTOTAL						30,408	841	
0020	94+12	-	106+30	RT	STH 73	462	--	PARKING STALLS
	95+19	-	106+30	LT	STH 73	462	--	PARKING STALLS
SUBTOTAL						924	--	
PROJECT TOTAL						31,332	841	

REMOVING DRAINAGE ITEMS

204.0220 204.0245
REMOVING INLETS REMOVING STORM SEWER (SIZE)

CATEGORY	STATION	OFFSET	ALIGNMENT	EACH	2-INCH LF	15-INCH LF
0030	107+00	RT	STH 73	1	12	12
PROJECT TOTAL				1	12	12

CURB RAMP REMOVALS

204.0100 204.0110 204.0150 204.0155

REMOVING CONCRETE PAVEMENT SY
REMOVING ASPHALTIC SURFACE SY
REMOVING CURB & GUTTER LF
REMOVING CONCRETE SIDEWALK SY

CATEGORY	STATION	OFFSET	TO	STATION	OFFSET	ALIGNMENT	SY	SY	LF	SY	COMMENTS	
0010	67+85	LT/RT	-	68+73	LT/RT	STH 73	--	32	143	59	WASHBURN RD	
	75+69	LT/RT	-	76+46	LT/RT	STH 73	--	13	57	33	SIMONSON BLVD	
	80+74	LT/RT	-	82+12	LT/RT	STH 73	--	20	92	75	QUARRY ST	
	84+97	LT/RT	-	86+62	LT/RT	STH 73	--	27	82	73	GREENWOOD ST/LAKE ST	
	89+25	LT/RT	-	90+64	LT/RT	STH 73	--	11	48	75	STATE ST	
	91+31	LT/RT	-	92+15	LT/RT	STH 73	18	14	65	52	LEGREID ST	
	94+06	LT/RT	-	94+76	LT/RT	STH 73	--	16	72	86	LIBERTY ST	
	103+57	LT/RT	-	103+84	LT/RT	STH 73	--	12	53	56	NELSON ST	
	106+30	LT/RT	-	107+24	LT/RT	STH 73	--	23	48	44	S. INDUSTRIAL PARK DR	
	108+25	LT/RT	-	108+58	LT/RT	STH 73	--	27	52	46	BIKE PATH	
	119+93	RT	-	120+86	RT	STH 73	--	6	27	21	N. INDUSTRIAL PARK DR	
	122+42	LT/RT	-	123+09	LT/RT	STH 73	--	12	54	31	TRUMAN DR	
	124+10	LT	-	124+99	LT	STH 73	--	7	32	18	NORTH ST	
							--	--	100	--		TEMP MID BLOCK CROSSING
	SUBTOTAL							18	220	925	669	
0030	10+00	LT/RT		10+92	LT/RT	'A'	--	15	72	12	S. INDUSTRIAL PARK ROAD, NE	
	SUBTOTAL							--	15	72	12	
PROJECT TOTAL							18	235	997	681		

EARTHWORK						
DIVISION	FROM/TO STATION	LOCATION	205.0100 COMMON EXCAVATION (1)	AVAILABLE MATERIAL (5)	MASS ORDINATE +/- (14)	WASTE
			CUT (2)			
DIVISION 1 - SOUTH SEGMENT CATEGORY 0010	81+60/87+60	STH 73	1,782	1,782	1,782	1,782
DIVISION 1 SUBTOTAL			1,782	1,782	1,782	1,782
DIVISION 2 - MIDDLE SEGMENT CATEGORY 0010	106+30/113+00	STH 73	2,158	2,158	2,158	2,158
DIVISION 2 SUBTOTAL			2,158	2,158	2,158	2,158
DIVISION 3 - NORTH SEGMENT CATEGORY 0010	119+50/125+83	STH 73	1,868	1,868	1,868	1,868
DIVISION 3 SUBTOTAL			1,868	1,868	1,868	1,868
DIVISION 4 - SOUTH INDUSTRIAL PARK RD CATEGORY 0030	10+14.928/10+83.167	SOUTH INDUSTRIAL PARK RD	73	73	73	73
DIVISION 4 SUBTOTAL			73	73	73	73
GRAND TOTAL			5,881	5,881	5,881	5,881
TOTAL COMMON EXC			5,881			

NOTES:

(1) COMMON EXCAVATION IS THE SUM OF THE CUT AND EBS EXCAVATION COLUMNS. ITEM NUMBER 205.0100

(2) SALVAGED/UNSUALE PAVEMENT MATERIAL IS INCLUDED IN CUT.

5) AVAILABLE MATERIAL = CUT - SALVAGED/UNUSUALE PAVEMENT MATERIAL

(14) THE MASS ORDINATE + OR - QTY CALCULATED FOR THE DIVISION. PLUS QUANTITY INDICATES AN EXCESS OF MATERIAL WITHIN THE DIVISION.

MINUS INDICATES A SHORTAGE OF MATERIAL WITHIN THE DIVISION.

CURB RAMP ITEMS

CATEGORY	STATION	OFFSET	TO	STATION	OFFSET	ALIGNMENT	416.0160	416.0260	465.0110	601.0411	602.0405	602.0415	601.0600	602.0505	602.0605	620.0300	COMMENTS
							CONCRETE DRIVEWAY 6-INCH SY	CONCRETE DRIVEWAY HES 6-INCH SY	ASPHALTIC SURFACE PATCHING TON	CONCRETE CURB & GUTTER 30-INCH TYPE D LF	CONCRETE SIDEWALK 4-INCH SF	CONCRETE SIDEWALK 6-INCH SF	CONCRETE CURB PEDESTRIAN LF	CURB RAMP DETECTABLE YELLOW SF	CURB RAMP DETECTABLE RADIAL YELLOW SF	CONCRETE MEDIAN SLOPED NOSE SF	
0010	67+85	LT/RT	-	68+73	LT/RT	STH 73	--	--	11	143	736	--	--	70	--	32	WASHBURN RD
	75+69	LT/RT	-	76+46	LT/RT	STH 73	--	22	4	57	300	--	7	40	--	--	SIMONSON BLVD
	80+74	LT/RT	-	82+12	LT/RT	STH 73	--	--	7	92	875	--	34	60	--	--	QUARRY ST
	84+97	LT/RT	-	86+62	LT/RT	STH 73	--	--	6	82	890	--	23	60	--	--	GREENOOD ST/LAKE ST
	89+25	LT/RT	-	90+64	LT/RT	STH 73	--	--	4	48	925	--	54	40	--	--	STATE ST
	91+31	LT/RT	-	92+15	LT/RT	STH 73	--	--	5	65	575	--	--	40	--	--	LEGREID ST
	94+06	LT/RT	-	94+76	LT/RT	STH 73	--	--	6	72	727	--	35	40	--	--	LIBERTY ST
	103+57	LT/RT	-	103+84	LT/RT	STH 73	--	--	4	53	500	--	--	20	--	--	NELSON ST
	106+30	LT/RT	-	107+24	LT/RT	STH 73	--	--	5	105	411	--	28	20	20	--	S. INDUSTRIAL PARK DR
	108+25	LT/RT	-	108+58	LT/RT	STH 73	--	--	4	52	285	306	--	60	--	--	BIKE PATH
	119+93	RT	-	120+86	RT	STH 73	--	--	2	27	190	--	--	20	--	--	N. INDUSTRIAL PARK DR
	122+42	LT/RT	-	123+09	LT/RT	STH 73	--	--	4	54	405	--	18	40	--	--	TRUMAN DR
	124+10	LT	-	124+99	LT	STH 73	--	--	2	32	225	--	--	100	16	--	NORTH ST
							--	--	--	100	--	--	--	--	--	--	TEMP MID BLOCK CROSSING
SUBTOTAL							--	22	64	982	7,044	306	199	610	36	32	
0030	10+00	LT/RT		10+92	LT/RT	'A'	12	--	3	69	161	--	--	10	--	--	S. INDUSTRIAL PARK ROAD, NE
SUBTOTAL							12	--	3	69	161	--	--	10	--	--	
PROJECT TOTAL							12	22	67	1,051	7,205	306	199	620	36	32	

PREPARE FOUNDATION ITEMS

211.0101

PREPARE
FOUNDATION FOR
ASPHALTIC PAVING
(PROJECT) 3070-04-60

CATEGORY	LOCATION	STATION TO	STATION	ALIGNMENT	EACH	COMMENTS
0010	STH 73				1	
PROJECT TOTAL					1	

BASE AGGREGATE ITEMS

305.0110 305.0120 305.0130
BASE AGGREGATE DENSE BASE AGGREGATE DENSE BASE AGGREGATE DENSE

CATEGORY	LOCATION	STATION	OFFSET	TO	STATION	OFFSET	ALIGNMENT	305.0110	305.0120	305.0130	COMMENTS
								3/4-INCH TON	1 1/4-INCH TON	3-INCH TON	
0010	STH 73 MAINLINE	32+74	LT/RT	-	61+06	LT/RT	STH 73	489	--	--	SHOULDERS
	STH 73 MAINLINE	81+60	LT/RT	-	87+60	LT/RT	STH 73	--	1,422	1,422	
	STH 73 MAINLINE	106+30	LT/RT	-	113+00	LT/RT	STH 73	--	1,693	1,693	
	STH 73 MAINLINE	119+50	LT/RT	-	125+83	LT/RT	STH 74	--	1,500	1,500	
SUBTOTAL								489	4,615	4,615	
0030	SOUTH INDUSTRIAL	10+00	RT/LT	-	10+92	RT/LT	'A'	--	61	61	NE QUADRANT
SUBTOTAL								--	61	61	
PROJECT TOTAL								489	4,676	4,676	

ASPHALTIC ITEMS

CATEGORY	STATION	TO	STATION	OFFSET	ALIGNMENT	455.0605	460.6223	460.6224	465.0105	465.0125	465.0475	COMMENTS
						TACK COAT GAL	HMA PAVEMENT 3 MT 58-28 S TON	HMA PAVEMENT 4 MT 58-28 S TON	ASPHALTIC SURFACE TON	ASPHALTIC SURFACE TEMPORARY TON	ASPHALTIC CENTERLINE RUMBLE STRIPS 2-LANE RURAL LF	
0010	32+74	-	61+06	RT/LT	STH 73	1,322	1,189	925	--	--	2,832	STH 73 MILL & OVERLAY
	61+06	-	81+60	RT/LT	STH 73	1,022	920	716	--	--	--	STH 73 MILL & OVERLAY
	87+60	-	95+00	RT/LT	STH 73	368	332	258	--	--	--	STH 73 MILL & OVERLAY
	95+00	-	106+30	RT/LT	STH 73	655	589	458	--	--	--	STH 73 MILL & OVERLAY
	113+00	-	119+50	RT/LT	STH 73	324	291	227	--	--	--	STH 73 MILL & OVERLAY
	81+60	-	87+60	RT/LT	STH 73	303	273	212	243.0	--	--	STH 73 FULL DEPTH
	106+30	-	113+00	RT/LT	STH 73	365	329	256	292	--	--	STH 73 FULL DEPTH
	119+50	-	125+83	RT/LT	STH 73	321	289	225	257	--	--	STH 73 FULL DEPTH
	52+04	-	61+06	LT	STH 73	111	100	78	--	--	--	PASSING LANE
	52+83	-	57+79	RT	STH 73	83	75	58	--	--	--	LONDON RD
	67+77	-	71+25	LT	STH 73	45	40	31	--	--	--	WASHBURN RD, TURN LANE
	75+23	-	76+57	LT	STH 73	29	26	20	--	--	--	SIMONSON BLVD
	75+66	-	76+47	RT	STH 73	17	15	12	--	--	--	SIMONSON BLVD
	80+99	-	81+72	LT	STH 73	14	13	10	--	--	--	QUARRY ST
	81+34	-	82+07	RT	STH 73	13	12	9	--	--	--	QUARRY ST
	85+04	-	85+74	LT	STH 73	14	13	10	--	--	--	GREENWOOD ST
	85+74	-	86+65	RT	STH 73	18	17	13	--	--	--	LAKE ST
	89+49	-	90+31	LT	STH 73	16	14	11	--	--	--	STATE ST
	91+32	-	92+15	RT	STH 73	19	17	14	--	--	--	LEGREID ST
	93+77	-	95+19	LT	STH 73	59	54	42	--	--	--	LIBERTY ST
	98+02	-	98+56	LT	STH 73	7	7	5	--	--	--	DEERFIELD ST
	98+02	-	98+56	RT	STH 73	9	8	6	--	--	--	DEERFIELD ST
	103+18	-	10+37	RT	STH 73	11	10	8	--	--	--	NELSON ST
	103+18	-	10+37	RT	STH 73	9	8	6	--	--	--	NELSON ST
	106+29	-	107+00	RT	STH 73	26	23	18	--	--	--	S. INDUSTRIAL PARK RD
	119+94	-	120+85	RT	STH 73	31	28	22	--	--	--	N. INDUSTRIAL PARK RD
	122+50	-	122+94	LT	STH 73	8	7	5	--	--	--	TRUMAN DR
	124+02	-	125+00	LT	STH 73	26	24	18	--	--	--	NORTH ST
	PROJECT 3070-04-60					--	--	--	--	5	--	MANHOLES
0010	SUBTOTAL					5,245	4,720	3,672	792	5	2,832	
0020	95+19	-	106+30	LT	STH 73	65	58	45	--	--	--	PARKING STALLS
0030	10+00	-	10+92	RT/LT	'A'	4	4	3	3	--	--	S INDUSTRIAL INTERSECTION
	PROJECT TOTAL					5,314	4,782	3,720	795	5	2,832	

STORM SEWER ITEMS

608.0315

STORM SEWER PIPE
REINFORCED CONCRETE
CLASS III 15-INCH

3

CATEGORY	LOCATION	FROM STRUCTURE	TO STRUCTURE	INLET ELEVATION	DISCHARGE ELEVATION	SLOPE	LF
0030	S INDUSTRIAL PARK ROAD	P-1.1	P-1.2	857.66	857.10	1.63%	36
	S INDUSTRIAL PARK ROAD	P-1.2	P-1.3	857.10	856.13	1.78%	54
SUBTOTAL							90
PROJECT TOTAL							90

RESTORATION ITEMS

625.0105 TOPSOIL CY
631.0300 SOD WATER MGAL
631.1000 SOD LAWN SY

CATEGORY	LOCATION	OFFSET	ALIGNMENT	625.0105 TOPSOIL CY	631.0300 SOD WATER MGAL	631.1000 SOD LAWN SY
0010	WASHBURN ROAD	LT	STH 73	1	0.2	10
	WASHBURN ROAD	RT	STH 73	5	2.0	90
	SIMONSON STREET	RT	STH 73	1	0.2	10
	QUARRY STREET	LT	STH 73	3	1.2	60
	GREENWOOD STREET	LT	STH 73	2	0.6	30
	LAKE STREET	RT	STH 73	1	0.3	20
	STATE STREET	LT	STH 73	3	1.2	60
	LEGREID STREET	RT	STH 73	1	0.2	10
	S INDUSTRIAL PARK ROAD	RT	STH 73	1	0.3	20
	GLACIAL DRUMLIN BIKE TRAIL	LT	STH 73	1	0.3	20
	N INDUSTRIAL PARK ROAD	RT	STH 73	1	0.2	20
	TRUMAN DRIVE	LT/RT	STH 73	2	0.6	30
	NORTH STREET	LT	STH 73	1	0.2	10
	UNDISTRIBUTED		STH 73	5	1.8	95
PROJECT TOTAL				28	9.3	485

3

STORM SEWER STRUCTURES

611.3004 INLETS 4-FT DIAMETER EACH
611.8110 ADJUSTING MANHOLE COVERS EACH
611.0624 INLET COVERS TYPE H EACH
611.9710 SALVAGED INLET COVERS EACH

CATEGORY	LOCATION	STATION	OFFSET	ALIGNMENT	STRUCTURE NUMBER	RIM ELEVATION	STRUCTURE ELEVATION	BOTTOM OF STRUCTURE ELEVATION	DEPTH	611.3004 INLETS 4-FT DIAMETER EACH	611.8110 ADJUSTING MANHOLE COVERS EACH	611.0624 INLET COVERS TYPE H EACH	611.9710 SALVAGED INLET COVERS EACH
0010	STH 73									--	11	--	--
SUBTOTAL										--	11	--	--
0030	S INDUSTRIAL PARK ROAD	106+33	24.8' RT	STH 73	P-1.1	860.66	860.16	857.66	2.50	1	--	1	--
	S INDUSTRIAL PARK ROAD	106+62	43.1' RT	STH 73	P-1.2	860.77	860.27	857.10	3.17	1	--	1	--
	S INDUSTRIAL PARK ROAD	107+13	23.2' RT	STH 73	P-1.3	859.77	859.27	856.13	3.14	1	--	--	1
SUBTOTAL										3	--	2	1
PROJECT TOTAL										3	11	2	1

NOTE

- 1) STATIONS SHOWN ARE TO THE CENTER OF STRUCTURE
- 2) OFFSETS ARE TO THE FLANGE LINE FOR STRUCTURES IN THE ROADWAY WITH INLET GRATES AND TO THE CENTER OF COVER FOR FIELD INLETS AND MANHOLES WITH SOLID LIDS
- 3) RIM ELEVATIONS ARE GIVEN AT THE FLANGE LINE FOR INLET GRATES OR THE CENTER OF THE MANHOLE COVER FOR MANHOLES
- 4) DEPTH = RIM ELEVATION - INVERT - CASTING HEIGHT - ADJUSTMENT

INLET PROTECTION

628.7005 INLET PROTECTION TYPE A EACH
628.7015 INLET PROTECTION TYPE C EACH

EROSION CONTROL MOBILIZATION

628.1905 MOBILIZATIONS EROSION CONTROL EACH
628.1910 EMERGENCY EROSION CONTROL EACH

CATEGORY	LOCATION	628.1905 MOBILIZATIONS EROSION CONTROL EACH	628.1910 EMERGENCY EROSION CONTROL EACH
0010	PROJECT 3070-04-60	5	5
SUBTOTAL		5	5
PROJECT TOTAL		5	5

CATEGORY	LOCATION	628.7005 INLET PROTECTION TYPE A EACH	628.7015 INLET PROTECTION TYPE C EACH	COMMENTS
0010	STH 73	--	34	
SUBTOTAL		--	34	
0030	S INDUSTRIAL PARK ROAD	3	3	NE QUADRANT
SUBTOTAL		3	3	
PROJECT TOTAL		3	37	

TYPE II SIGNS AND SUPPORTS

634.0614* 637.2210 637.2230

CATEGORY	SIGN NUMBER	SIGN CODE	SIGN SIZE	SIGN TYPE	SIGN DIMENSION W X H			DESCRIPTION	POSTS WOOD 4X6-INCH X 14-FT EACH	SIGNS TYPE II REFLECTIVE H SF	SIGNS TYPE II REFLECTIVE F SF	COMMENTS
					IN	X	IN					
0010	6-1	R9-3A	2S	II	24	X	24	DO NOT WALK	1	4.0	--	
	6-2	R9-3BR	2	II	18	X	12	USE CROSSWALK & RIGHT ARROW	--	1.5	--	SIGN MOUNTED WITH 6-1
	6-3	R9-3A	2S	II	24	X	24	DO NOT WALK	1	4.0	--	
	6-4	R9-3BL	2	II	18	X	12	USE CROSSWALK & LEFT ARROW	--	1.5	--	SIGN MOUNTED WITH 6-3
	6-5	R9-3A	2S	II	24	X	24	DO NOT WALK	1	4.0	--	
	6-6	R9-3BL	2	II	18	X	12	USE CROSSWALK & LEFT ARROW	--	1.5	--	SIGN MOUNTED WITH 6-5
	6-7	R9-3A	2S	II	24	X	24	DO NOT WALK	1	4.0	--	
	6-8	R9-3BL	2	II	18	X	12	USE CROSSWALK & LEFT ARROW	--	1.5	--	SIGN MOUNTED WITH 6-7
	6-9	R9-3A	2S	II	24	X	24	DO NOT WALK	1	4.0	--	
	6-10	R9-3BR	2	II	18	X	12	USE CROSSWALK & RIGHT ARROW	--	1.5	--	SIGN MOUNTED WITH 6-9
	6-11	W13-1	2S	II	18	X	18	15 MPH	--	--	2.25	SIGN MOUNTED ON EXISTING POST
7-1	R9-3A	2S	II	24	X	24	DO NOT WALK	1	4.0	--		
7-2	R9-3BL	2	II	18	X	12	USE CROSSWALK & LEFT ARROW	--	1.5	--	SIGN MOUNTED WITH 7-1	
7-3	W13-1	2S	II	18	X	18	15 MPH	--	--	2.25	SIGN MOUNTED ON EXISTING POST	
10-1	R9-3A	2S	II	24	X	24	DO NOT WALK	1	4.0	--		
10-2	R9-3BR	2	II	18	X	12	USE CROSSWALK & RIGHT ARROW	--	1.5	--	SIGN MOUNTED WITH 10-1	
SUBTOTAL								7	39	4.50		
ADDITIONAL QUANTITIES ELSEWHERE								1	--	--	SEE REMOVING AND MOVING SIGNS	
CATEGORY TOTAL								8	39	4.50		
0030	ADDITIONAL QUANTITIES ELSEWHERE								1	--	--	SEE REMOVING AND MOVING SIGNS
CATEGORY TOTAL								1	--	--		
PROJECT TOTAL								9	39	4.50		

REMOVING AND MOVING SIGNS

634.0614* 638.2102 638.3000

CATEGORY	LOCATION	STATION	OFFSET	ALIGNMENT	SIGN NUMBER	SIGN MESSAGE	POSTS WOOD 4X6-INCH X 14-FT EACH	MOVING SIGNS TYPE II EACH	REMOVING SMALL SIGN SUPPORTS EACH	COMMENTS	
											0010
	STH 73	76+44	RT	STH 73	S1-1	PED CROSSING	--	1	1	SIGN TO BE ATTACHED TO LIGHT POST	
	STH 73	76+44	RT	STH 73	S16-7L	PED CROSSING ARROW	--	1	1	SIGN TO BE ATTACHED TO LIGHT POST	
SUBTOTAL							1	3	3		
ADDITIONAL QUANTITIES ELSEWHERE							6	--	--	SEE TYPE II SIGNS AND SUPPORTS	
0010	CATEGORY TOTAL							7	3	3	
0030	S INDUSTRIAL PARK ROAD	107+00	RT	STH 73	R1-1	STOP	1	1	1		
SUBTOTAL							1	1	1		
PROJECT TOTAL							8	4	4		

TRAFFIC CONTROL ITEMS-DETOUR

643.0420*

643.0705*

643.0900*

643.0910

643.0920

TRAFFIC CONTROL COVERING SIGNS TYPE I

TRAFFIC CONTROL COVERING SIGNS TYPE II

TRAFFIC CONTROL BARRICADES TYPE III

TRAFFIC CONTROL WARNING LIGHTS TYPE A

TRAFFIC CONTROL SIGNS

CATEGORY	STAGE	LOCATION	DURATION		CYCLES		TRAFFIC CONTROL BARRICADES TYPE III		TRAFFIC CONTROL WARNING LIGHTS TYPE A		TRAFFIC CONTROL SIGNS		TRAFFIC CONTROL COVERING SIGNS TYPE I	TRAFFIC CONTROL COVERING SIGNS TYPE II	COMMENTS
			DAY	NO.	NO.	DAY	NO.	DAY	NO.	DAY	NO.	DAY	EACH	EACH	
0010	1	STH 73 & US 12/18	7	1	2	14	4	28	20	140	--	29		DETOUR CTH E-DETAIL 1	
		CTH W/OAK PARK ROAD & US 12/18	7	1	--	--	--	--	6	42	--	--		DETOUR CTH E-DETAIL 2	
		CTH BN/NORA ROAD & US 12/18	7	1	--	--	--	--	6	42	--	--		DETOUR CTH E-DETAIL 3	
		CTH N & US 12/18	7	1	--	--	--	--	21	147	--	--		DETOUR CTH E-DETAIL 4	
		CTH AB & US 12/18	7	1	--	--	--	--	6	42	--	--		DETOUR CTH E-DETAIL 5	
		IH 39/IH 90 & US 12/18	7	1	--	--	--	--	63	441	--	12		DETOUR CTH E-DETAIL 6	
		WI & SOUTHERN RAILROAD & IH 39/IH 90	7	1	--	--	--	--	3	21	--	--		DETOUR CTH E-DETAIL 7	
		CTH BB & IH 39/IH 90	7	1	--	--	--	--	3	21	--	--		DETOUR CTH E-DETAIL 8	
		IH 39/IH 90 & STH 30/IH 94	7	1	--	--	--	--	79	553	--	--		DETOUR CTH E-DETAIL 9	
		CTH N & IH 94	7	1	--	--	--	--	6	42	--	--		DETOUR CTH E-DETAIL 10	
		IH 94 & BAXTER ROAD	7	1	--	--	--	--	3	21	--	--		DETOUR CTH E-DETAIL 11	
		STH 73 & IH 94	7	1	2	14	4	28	35	245	3	27		DETOUR CTH E-DETAIL 12	
		IH 94 & MISSOURI ROAD	7	1	--	--	--	--	14	98	1	--		DETOUR CTH E-DETAIL 13	
		CTH BB & STH 73	7	1	--	--	--	--	20	140	--	6		DETOUR CTH E-DETAIL 14	
		SUBTOTAL				28		56		1,995	4	74			
		ADDITIONAL QUANTITIES ELSEWHERE				745		57		3,814	--	--		SEE TEMPORARY PEDESTRIAN DETOUR	
		ADDITIONAL QUANTITIES ELSEWHERE				168		336		2,702	--	--		SEE TRAFFIC CONTROL ITEMS	
		PROJECT TOTAL				941		449		8,511	4	74			

*ADDITIONAL QUANTITIES SHOWN ELSEWHERE

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		TRAFFIC CONTROL ITEMS													
				643.0420		643.0705		643.0900		643.1050		643.5000			
				TRAFFIC CONTROL BARRICADES TYPE III		TRAFFIC CONTROL WARNING LIGHTS TYPE A		TRAFFIC CONTROL SIGNS		TRAFFIC CONTROL SIGNS PCMS		TRAFFIC CONTROL EACH			
CATEGORY	STAGE	LOCATION	DURATION DAYS	CYCLES NO.	NO.	DAY	NO.	DAY	NO.	DAY	NO.	DAY	NO.	DAY	COMMENTS
0010	1	BEGIN/END PROJECT	7	2	--	--	--	--	--	--	2	28	--	--	ADVANCE WARNING PCMS
		STH 73	73	1	--	--	--	--	10	730	--	--	--	--	TRAFFIC CONTROL STH 73 OPEN TO TRAFFIC
		STH 73	7	1	8	56	16	112	16	112	--	--	--	--	TRAFFIC CONTROL STH 73 CLOSED TO THROUGH TRAFFIC
		SIDEROADS	73	1	--	--	--	--	19	1,387	--	--	--	--	TRAFFIC CONTROL STH 73 OPEN TO TRAFFIC
		SIDEROADS	7	1	16	112	32	224	59	413	--	--	--	--	TRAFFIC CONTROL STH 73 CLOSED TO THROUGH TRAFFIC
		STH 73	10	1	--	--	--	--	4	40	--	--	--	--	BUMP (W08-1), ARROW (W016-7L)
		STH 73	10	1	--	--	--	--	2	20	--	--	--	--	UNEVEN LANE (W8-11)
		PROJECT 3070-04-60	--	--	--	--	--	--	--	--	--	--	--	--	
		SUBTOTAL				168		336		2,702		28		1	
		ADDITIONAL QUANTITIES ELSEWHERE				28		56		1,995		--		--	SEE TRAFFIC CONTROL ITEMS-DETOUR
		ADDITIONAL QUANTITIES ELSEWHERE				745		57		3,814		--		--	SEE TEMPORARY PEDESTRIAN DETOUR
		PROJECT TOTAL				941		449		8,511		28		1	

*ADDITIONAL QUANTITIES SHOWN ELSEWHERE

		TEMPORARY PEDESTRIAN DETOUR																
		643.0300		643.0420*		643.0705*		643.0715		643.0900*		644.1430		644.1601		644.1605		
		TRAFFIC CONTROL DRUMS		TRAFFIC CONTROL BARRICADES TYPE III		TRAFFIC CONTROL WARNING LIGHTS TYPE A		TRAFFIC CONTROL WARNING LIGHTS TYPE C		TRAFFIC CONTROL SIGNS		TEMPORARY PEDESTRIAN SURFACE PLATE		TEMPORARY PEDESTRIAN CURB RAMP		TEMPORARY PEDESTRIAN DETECTABLE WARNING FIELD		
CATEGORY	STAGE	DURATION DAY	CYCLES NO.	NO.	DAY	NO.	DAY	NO.	DAY	NO.	DAY	NO.	DAY	SF	NO.	DAY	SF	COMMENTS
0010	1	3	1	60	180	3	9	2	6	10	30	10	30	60	4	12	40	WASHBURN RD, LANE SHIFT
		3	1	30	90	4	12	2	6	5	15	11	33	150	2	6	24	SIMONSON BLVD, LANE SHIFT
		19	1	--	--	25	475	--	--	--	--	89	1,691	1,300	16	304	128	ALL OTHER INTERSECTIONS
		STAGE 1 SUBTOTAL			270		496		12		45		1,754	1,510		322	192	--
	2	3	1	34	102	6	18	2	6	5	15	16	48	195	3	9	40	SIMONSON BLVD, LANE SHIFT
		3	1	16	48	15	45	2	6	5	15	26	78	260	4	12	48	NORTH STREET, LANE SHIFT
		19	1	--	--	--	--	--	--	--	--	65	1,235	130	--	--	--	ALL OTHER INTERSECTIONS
		STAGE 2 SUBTOTAL			150		63		12		30		1,361	585		21	88	
	3	8	1	--	--	13	104	--	--	--	--	30	240	--	--	--	--	ALL OTHER INTERSECTIONS
		7	1	--	--	2	14	4	28	--	--	16	112	--	--	--	--	TRAIL DETOUR
		STAGE 3 SUBTOTAL			--		118		28		--		352	--	--	--	--	
		UNDISTRIBUTED	1	--	42	--	68	--	5	--	8	--	347	210		34	28	
		SUBTOTAL			462		745		57		83		3,814	2,305		377	308	
		ADDITIONAL QUANTITIES ELSEWHERE			--		168		336		--		2,702	--		--	--	SEE TRAFFIC CONTROL ITEMS
		ADDITIONAL QUANTITIES ELSEWHERE			--		28		56		--		1,995	--		--	--	SEE TRAFFIC CONTROL ITEMS - DETOUR
		PROJECT TOTAL			462		941		449		83		8,511	2,305		377	308	

*ADDITIONAL QUANTITIES SHOWN ELSEWHERE

PAVEMENT MARKING ITEMS

CATEGORY	LOCATION	STATION	TO	STATION	ALIGNMENT	643.3120		646.1040		646.3040	646.4520		646.5020	646.5220
						TEMPORARY MARKING LINE EPOXY 4-INCH		MARKING LINE GROOVED WET REF EPOXY 4-INCH		MARKING LINE GROOVED WET REF EPOXY 8-INCH	MARKING LINE SAME DAY EPOXY 4-INCH		MARKING ARROW EPOXY EACH	MARKING SYMBOL EPOXY EACH
						12.5' LINE 37.5'		12.5' LINE 37.5'		SOLID WHITE	12.5' LINE 37.5'			
						SKIP YELLOW LF	SOLID YELLOW LF	SKIP WHITE LF	SOLID WHITE LF		SOLID WHITE LF	SKIP YELLOW LF	SOLID YELLOW LF	
0010	STH 73 MAINLINE	55+02	-	57+57	STH 73	--	--	75	--	--	--	--	--	--
	STH 73 MAINLINE	67+98	-	70+26	STH 73	--	--	--	160	--	--	--	--	--
	STH 73 MAINLINE	32+73	-	61+06	STH 73	--	5,670	--	5,100	--	5,670	--	--	--
	STH 73 MAINLINE	61+06	-	68+58	STH 73	--	1,510	--	--	--	1,510	--	--	--
	STH 73 MAINLINE	68+64	-	76+35	STH 73	200	--	--	--	--	200	--	--	--
	STH 73 MAINLINE	76+41	-	81+96	STH 73	150	163	--	--	--	150	163	--	--
	STH 73 MAINLINE	82+02	-	94+15	STH 73	100	2,065	--	--	--	100	2,065	--	--
	STH 73 MAINLINE	94+21	-	97+83	STH 73	--	730	--	130	--	730	1	--	--
	STH 73 MAINLINE	98+75	-	102+97	STH 73	--	850	--	430	--	850	3	--	--
	STH 73 MAINLINE	103+68	-	106+35	STH 73	--	540	--	270	--	540	2	--	--
	STH 73 MAINLINE	106+39	-	108+38	STH 73	--	400	--	120	--	400	1	--	--
	STH 73 MAINLINE	108+51	-	125+83	STH 73	--	3,470	--	--	--	3,470	--	--	--
	CATEGORY TOTAL					450	15,398	75	6,050	160	450	15,398	7	--
0020	STH 73 MAINLINE	94+21	-	97+83	STH 73	--	--	--	--	--	--	--	--	1
	STH 73 MAINLINE	98+75	-	102+97	STH 73	--	--	--	--	--	--	--	--	4
	STH 73 MAINLINE	103+67	-	106+35	STH 73	--	--	--	--	--	--	--	--	2
	STH 73 MAINLINE	106+40	-	108+37	STH 73	--	--	--	--	--	--	--	--	1
	CATEGORY TOTAL					--	--	--	--	--	--	--	--	8
	PROJECT TOTAL					15,848		6,125		160	15,848		7	8

PAVEMENT MARKING ITEMS CONTINUED

CATEGORY	LOCATION	STATION	TO	STATION	ALIGNMENT	646.6120	646.7420	646.8120	646.8220	646.8320
						MARKING STOP LINE EPOXY 18-INCH LF	MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH LF	MARKING CURB EPOXY LF	MARKING ISLAND NOSE EPOXY EACH	MARKING PARKING STALL EPOXY LF
						0010	STH 73 MAINLINE	67+98	-	70+26
	STH 73 MAINLINE	75+88	-	76+42	STH 73	15	147	--	--	--
	STH 73 MAINLINE	81+18	-	82+02	STH 73	25	186	--	--	--
	STH 73 MAINLINE	85+21	-	86+47	STH 73	32	152	--	--	--
	STH 73 MAINLINE	89+67	-	90+08	STH 73	15	72	--	--	--
	STH 73 MAINLINE	91+55	-	91+98	STH 73	15	75	--	--	--
	STH 73 MAINLINE	94+12	-	94+67	STH 73	15	141	--	--	--
	STH 73 MAINLINE	97+83	-	98+75	STH 73	30	269	--	--	--
	STH 73 MAINLINE	102+97	-	103+68	STH 73	32	321	--	--	--
	STH 73 MAINLINE	106+35	-	107+05	STH 73	20	193	--	--	--
	STH 73 MAINLINE	108+37	-	108+52	STH 73	--	71	--	--	--
	STH 73 MAINLINE	120+13	-	120+65	STH 73	21	95	--	--	--
	STH 73 MAINLINE	122+62	-	122+82	STH 73	8	34	--	--	--
	STH 73 MAINLINE	124+26	-	124+90	STH 73	22	110	--	--	--
	STH 73 MAINLINE	32+73	-	61+06	STH 73	43	--	--	--	--
	SUBTOTAL					335	2,097	45	1	--
0020	STH 73 MAINLINE	94+21	-	97+83	STH 73	--	--	--	--	185
	STH 73 MAINLINE	98+75	-	102+97	STH 73	--	--	--	--	344
	STH 73 MAINLINE	103+67	-	106+35	STH 73	--	--	--	--	138
	STH 73 MAINLINE	106+40	-	108+37	STH 73	--	--	--	--	18
	CATEGORY TOTAL					--	--	--	--	685
	PROJECT TOTAL					335	2,097	45	1	685

TEMPORARY PEDESTRIAN DETOUR PAVEMENT MARKING

CATEGORY	STAGE	643.3960	643.3970	643.3150		COMMENTS
		TEMPORARY MARKING REMOVABLE MASK OUT TAPE 6-INCH	TEMPORARY MARKING REMOVABLE MASK OUT TAPE 10-INCH	TEMPORARY MARKING LINE REMOVABLE TAPE 4-INCH		
		LF	LF	SOLID WHITE	SOLID YELLOW	
0010	1	610	170	900	450	WASHBURN RD, LANE SHIFT
		400	--	800	400	SIMONSON BLVD, LANE SHIFT
	STAGE 1 SUBTOTAL	1,010	170	1,700	850	
	2	--	--	800	400	SIMONSON BLVD, LANE SHIFT
		270	--	540	270	NORTH STREET, LANE SHIFT
	STAGE 2 SUBTOTAL	270	--	1,340	670	
	PROJECT TOTAL	1,280	170	4,560		

STAKING ITEMS

CATEGORY	LOCATION	650.4000	650.4500	650.5000	650.8000	650.9000	650.9500	650.9911
		CONSTRUCTION STAKING STORM SEWER EACH	CONSTRUCTION STAKING SUBGRADE LF	CONSTRUCTION STAKING BASE LF	CONSTRUCTION STAKING RESURFACING REFERENCE LF	CONSTRUCTION STAKING CURB RAMP EACH	CONSTRUCTION STAKING SIDEWALK (PROJECT) EACH	CONSTRUCTION STAKING SUPPLEMENTAL CONTROL (PROJECT) EACH
		0010	STH 73	--	1,900	1,900	9,300	42
	SUBTOTAL	--	1,900	1,900	9,300	42	1	1
0030	STH 73	3	--	--	--	--	--	--
	SUBTOTAL	3	--	--	--	--	--	--
	PROJECT TOTAL	3	1,900	1,900	9,300	42	1	1

GUARDRAIL ITEMS

CATEGORY	STAGE	LOCATION	STATION	OFFSET	TO	STATION	OFFSET	ALIGNMENT	614.0400	614.095	614.0951	614.0952
									ADJUSTING STEEL PLATE BEAM GUARD LF	REPLACING GUARDRAIL POSTS AND BLOCKS EACH	REPLACING GUARDRAIL RAIL AND HARDWARE LF	REPLACING GUARDRAIL REFLECTORS EACH
									0010	1	STH 73	32+74
		STH 73	55+28	LT	-	61+06	LT	STH 73	580	--	--	--
						UNDISTRIBUTED				20	100	8
						SUBTOTAL			800	20	100	8
						PROJECT TOTAL			800	20	100	8

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SPECIALS

CATEGORY	LOCATION	SPV.0060.01	SPV.0060.02	SPV.0060.03	SPV.0060.04	SPV.0060.05	SPV.0090.01	SPV.0090.03	SPV.0195.01	COMMENTS
		MANAGEMENT OF CONTAMINATED GROUNDWATER EACH	RESEARCH AND LOCATE EXISTING LAND PARCEL MONUMENTS EACH	VERIFY AND REPLACE EXISTING LAND PARCEL MONUMENTS EACH	ADJUSTING WATER VALVE BOXES EACH	ADJUSTING SANITARY MANHOLE COVERS EACH	PROFILE CURB CUT LF	STORM SEWER PIPE PVC 2-INCH LF	EXCAVATION, HAULING, DISPOSAL PETROLEUM CONTAMINATED SOIL (DIRECT LANDFILL) TON	
0010	STH 73	--	17	17	--	--	--	--	--	TPP SHEET 4.01
	STH 73	--	44	44	--	--	--	--	--	TPP SHEET 4.02
	STH 73	--	34	34	--	--	--	--	--	TPP SHEET 4.03
	STH 73	--	28	28	--	--	--	--	--	TPP SHEET 4.04
	STH 73	--	18	18	--	--	--	--	--	TPP SHEET 4.05
	STH 73	--	16	16	--	--	--	--	--	TPP SHEET 4.06
	STH 73	1	--	--	12	12	105	--	5	
SUBTOTAL		1	157	157	12	12	105	--	5	
0030	STH 73	--	--	--	--	--	25	--	--	
	SUBTOTAL		--	--	--	--	--	25	--	
PROJECT TOTAL		1	157	157	12	12	105	25	5	

SAWING

CATEGORY	LOCATION	690.0150	690.0250	COMMENTS
		SAWING ASPHALT LF	SAWING CONCRETE LF	
0010	INTERSECTION	163	12	WASHBURN ROAD
	INTERSECTION	69	20	SIMONSON BLVD/STREET
	INTERSECTION	112	36	QUARRY STREET
	INTERSECTION	102	31	GREENWOOD STREET/LAKE STREET
	INTERSECTION	68	27	STATE STREET
	INTERSECTION	73	30	LEGREID STREET
	INTERSECTION	88	34	LIBERTY STREET
	INTERSECTION	61	60	NELSON STREET
	INTERSECTION	113	56	S INDUSTRIAL PARK ROAD
	INTERSECTION	60	31	GLACIAL DRUMLIN BIKE TRAIL
	INTERSECTION	35	8	N INDUSTRIAL PARK ROAD
	INTERSECTION	66	19	TRUMAN DRIVE
	INTERSECTION	40	8	NORTH STREET
	FULL DEPTH	830	--	PAVEMENT REPLACEMENT MATCH
	SUBTOTAL		1,880	472
0030	INTERSECTION	73	4	S INDUSTRIAL PARK ROAD, NE
	SUBTOTAL		73	4
PROJECT TOTAL		1,953	476	

MAINTENANCE AND REPAIR OF HAUL ROADS

CATEGORY	LOCATION	618.0100
		MAINTENANCE AND REPAIR OF HAUL ROADS (PROJECT) EACH
0010	PROJECT 3070-04-60	1
PROJECT TOTAL		1

WATER

CATEGORY	LOCATION	624.0100
		MGAL
0010	PROJECT 3070-04-60	94
PROJECT TOTAL		94

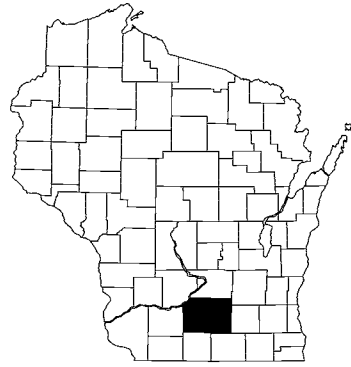
MOBILIZATION

CATEGORY	LOCATION	619.1000
		EACH
0010	PROJECT 3070-04-60	1
PROJECT TOTAL		1

FIELD OFFICE

CATEGORY	LOCATION	642.5201
		FIELD OFFICE TYPE C EACH
0010	PROJECT 3070-04-60	1
PROJECT TOTAL		1

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION TRANSPORTATION PROJECT PLAT TITLE SHEET 3070-04-20 I39 TO COLUMBUS SHAUL LANE TO NORTH STREET STH 73 DANE



CONVENTIONAL SYMBOLS

SECTION LINE	---	SECTION CORNER SYMBOL		R/W MONUMENT (TO BE SET)	●
QUARTER LINE	---	SECTION CORNER MONUMENT		NON-MONUMENTED R/W POINT	○
SIXTEENTH LINE	---	GEODETIC SURVEY MONUMENT		FOUND IRON PIN (1-INCH UNLESS NOTED)	IP ●
NEW REFERENCE LINE	---	SIXTEENTH CORNER MONUMENT		SIGN	
NEW R/W LINE	---	SIGN		OFF-PREMISE SIGN	
EXISTING R/W OR HE LINE	---	COMPENSABLE		NON-COMPENSABLE	
PROPERTY LINE	---	ELECTRIC POLE		TELEPHONE POLE	
LOT, TIE & OTHER MINOR LINES	---	PEDESTAL (LABEL TYPE) (TV, TEL, ELEC, ETC.)		ACCESS RESTRICTED BY ACQUISITION	
SLOPE INTERCEPT	---	NO ACCESS (BY STATUTORY AUTHORITY)		ACCESS RESTRICTED (BY PREVIOUS PROJECT OR CONTROL)	
CORPORATE LIMITS	---	NO ACCESS (NEW HIGHWAY)		PARCEL NUMBER (25)	
UNDERGROUND FACILITY (COMMUNICATIONS, ELECTRIC, ETC.)	---	UTILITY NUMBER (40)		PARALLEL OFFSETS	
NEW R/W (FEE OR HE) (HATCHING VARIES BY OWNER)	---	TO BE REMOVED			
TEMPORARY LIMITED EASEMENT AREA	---	BRIDGE			
EASEMENT AREA (PERMANENT LIMITED OR RESTRICTED DEVELOPMENT)	---				
TRANSMISSION STRUCTURES	---				

CONVENTIONAL ABBREVIATIONS

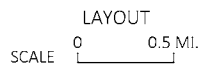
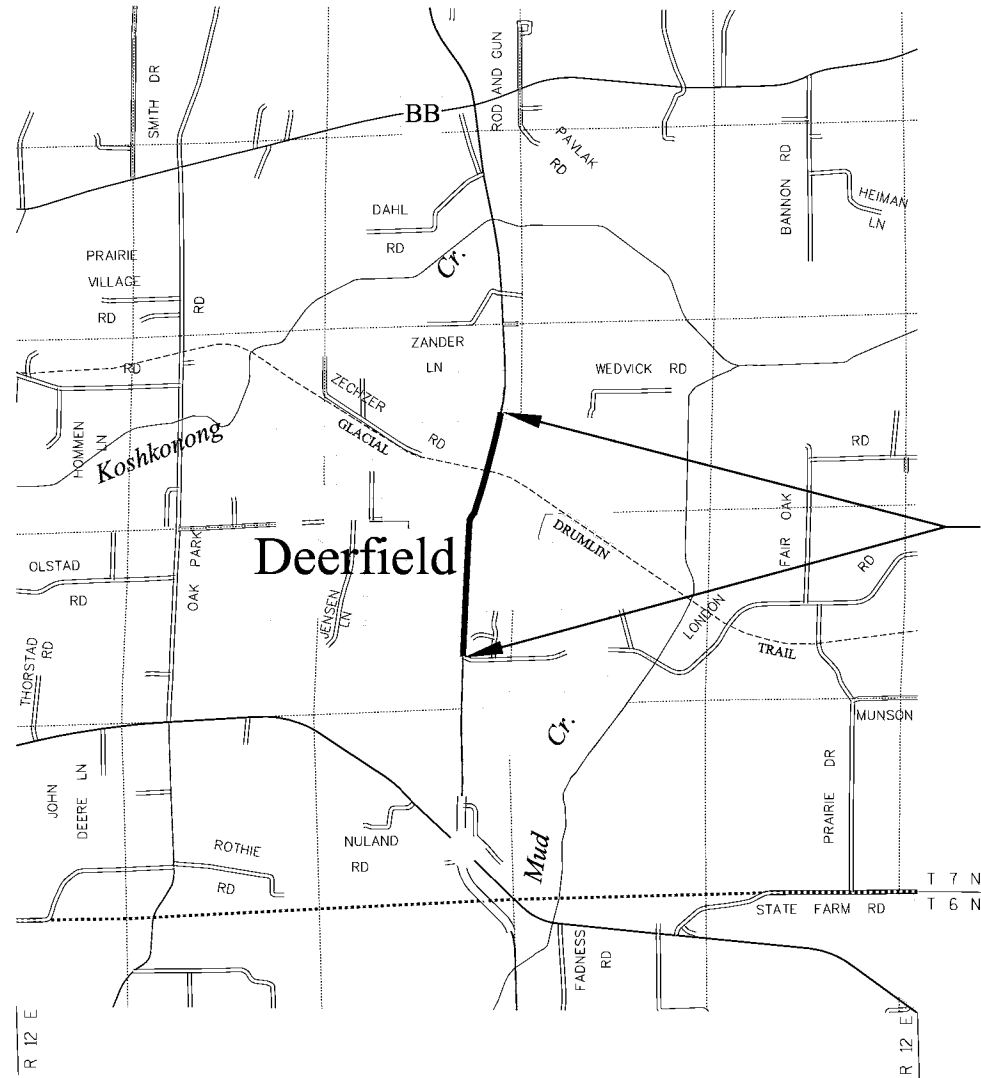
ACCESS RIGHTS	AR	POINT OF INTERSECTION	PI
ACRES	AC	PROPERTY LINE	PL
AHEAD	AH	RECORDED AS	(100')
ALUMINUM	ALUM	REEL / IMAGE	R/I
AND OTHERS	ET AL	REFERENCE LINE	R/L
BACK	BK	REMAINING	REM
BLOCK	BLK	RESTRICTIVE DEVELOPMENT	RDE
CENTERLINE	C/L	EASEMENT	
CERTIFIED SURVEY MAP	CSM	RIGHT	RT
CONCRETE	CONC	RIGHT OF WAY	R/W
COUNTY	CO	SECTION	SEC
COUNTY TRUNK HIGHWAY	CTH	SEPTIC VENT	SEPV
DISTANCE	DIST	SQUARE FEET	SF
CORNER	COR	STATE TRUNK HIGHWAY	STH
DOCUMENT NUMBER	DOC	STATION	STA
EASEMENT	EASE	TELEPHONE PEDESTAL	TP
EXISTING	EX	TEMPORARY LIMITED EASEMENT	TLE
GAS VALVE	GV	TRANSPORTATION PROJECT	TPP
GRID NORTH	GN	PLAT	
HIGHWAY EASEMENT	HE	UNITED STATES HIGHWAY	USH
IDENTIFICATION	ID	VOLUME	V
LAND CONTRACT	LC		
LEFT	LT		
MONUMENT	MON		
NATIONAL GEODETIC SURVEY	NGS		
NUMBER	NO		
OUTLOT	OL		
PAGE	P		
POINT OF TANGENCY	PT		
PERMANENT LIMITED EASEMENT	PLE		
POINT OF BEGINNING	POB		
POINT OF CURVATURE	PC		
POINT OF COMPOUND CURVE	PCC		

CURVE DATA

LONG CHORD	LCH
LONG CHORD BEARING	LCB
RADIUS	R
DEGREE OF CURVE	D
CENTRAL ANGLE	Δ/Delta
LENGTH OF CURVE	L
TANGENT	T
DIRECTION AHEAD	DA
DIRECTION BACK	DB

CONVENTIONAL UTILITY SYMBOLS

W	WATER
G	GAS
T	TELEPHONE
OH	OVERHEAD TRANSMISSION LINES
E	ELECTRIC
TV	CABLE TELEVISION
FO	FIBER OPTIC
SS	SANITARY SEWER
SS	STORM SEWER



THE NOTES, CONVENTIONAL SIGNS, AND ABBREVIATIONS ARE ASSOCIATED WITH EACH TRANSPORTATION PROJECT PLAT FOR PROJECT 3070-04-20

NOTES:

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), DANE COUNTY, NAD83(2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 3/4" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

ALL RIGHT-OF-WAY LINES DEPICTED IN THE NON-ACQUISITION AREAS ARE INTENDED TO RE-ESTABLISH EXISTING RIGHT-OF-WAY LINES AS DETERMINED FROM PREVIOUS PROJECTS, OTHER RECORDED DOCUMENTS, OR FROM CENTERLINE OF EXISTING PAVEMENTS.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS" OF PUBLIC RECORD.

DIMENSIONING FOR THE NEW RIGHT-OF-WAY IS MEASURED ALONG AND PERPENDICULAR TO THE NEW REFERENCE LINES.

A TEMPORARY LIMITED EASEMENT (TLE) IS A RIGHT FOR CONSTRUCTION PURPOSES, AS DEFINED HEREIN, INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON, THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM DESIRABLE. ALL (TLE'S) ON THIS PLAT EXPIRE AT THE COMPLETION OF THE CONSTRUCTION PROJECT FOR WHICH THIS INSTRUMENT IS GIVEN.

PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM DATA DERIVED FROM MAPS AND DOCUMENTS OF PUBLIC RECORD AND/OR EXISTING OCCUPATIONAL LINES. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.

FOR THE LATEST ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN MADISON, WI.

PARCEL AND UTILITY IDENTIFICATION NUMBERS MAY NOT POINT TO ALL AREAS OF ACQUISITION, AS NOTED ON THE TPP DETAIL PAGES.

INFORMATION FOR THE BASIS OF EXISTING HIGHWAY RIGHT-OF-WAY POINTS OF REFERENCE AND ACCESS CONTROL ARE LISTED ON THE TPP DETAIL PAGES.

PROJECT NUMBER 3070-04-20 -4.01
SHEET 2 OF 2
AMENDMENT NO:

TRANSPORTATION PROJECT PLAT NO: 3070-04-20-4.01

BEING PART OF LOT 1 OF DEERFIELD MEADOWS SUBDIVISION LOCATED IN PART OF THE NW1/4 OF THE SE1/4, PART OF LOT 10, BLOCK 4 OF GOVERNORS PLAT LOCATED IN PART OF THE SW1/4 OF THE NE1/4 AND PART OF LOT 4 OF CSM 763 LOCATED IN PART OF THE SE1/4 OF THE NE1/4, ALL IN SECTION 28, TOWNSHIP 7 NORTH, RANGE 12 EAST, VILLAGE OF DEERFIELD, DANE COUNTY, WISCONSIN

RELOCATION ORDER - 5TH 73, 139 TO COLUMBUS, SHAUL LANE TO NORTH STREET, DANE COUNTY

TO PROPERLY ESTABLISH LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE PROJECT.

TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SECTION 84.02 (1), 84.09, AND 84.30, WISCONSIN STATUTES, THE DEPARTMENT OF TRANSPORTATION HEREBY REQUESTS THAT THE LANDS OWNERS AND INTERESTS SHOWN ON THIS PLAT, LAY OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SHOWN FOR THE ABOVE PROJECT, 1. THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT, LAY OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SHOWN FOR THE ABOVE PROJECT, 2. THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE DEPARTMENT OF TRANSPORTATION AND SHALL BE ACQUIRED IN THE NAME OF THE STATE OF WISCONSIN, PURSUANT TO THE PROVISIONS OF SECTION 84.09 (1) OR (2), WISCONSIN STATUTES.

SCHEDULE OF LANDS & INTERESTS REQUIRED

OWNERS NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE DEPARTMENT.

PARCEL NUMBER	OWNERS	INTEREST REQUIRED	RAW SQ. FT. REQUIRED	TILE
1	D CLINIC, LLC	TILE	53	29
2	DONALD L DUJAN & DEBBIE L DUJAN	TILE	29	29
3	COUNTRY SQUARE APARTMENTS, LLC	TILE	350	29

POINT	Y (NORTHING)	X (EASTING)	DESCRIPTION
IP30120	471826.164	903260.205	RR SPIKE
IP30171	471379.944	903255.607	1.25" IRON ROD
IP30112	472345.715	903117.101	.75" IRON ROD
IP30113	471926.195	903264.671	.75" IRON ROD
IP30115	472277.975	903411.989	1.0" IRON ROD
IP30116	472252.190	903426.074	1.0" IRON ROD
IP30124	472259.957	903435.398	.75" IRON ROD
IP30125	472372.990	903416.874	1.25" O.D. IRON PIPE
IP30126	472673.983	903451.342	1.0" IRON ROD
IP30127	472087.749	903403.233	1.0" O.D. IRON PIPE

NOTES:
POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCONS), DANE COUNTY, NAD83 (2011) IN US SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES FOR THE LATEST ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN MADISON, WI.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 3/4"x24" IRON REBAR) UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE FOR STH73: GOVERNORS PLAT, DEERFIELD MEADOWS, CENTER STREET HILL PLAT, CSM 12446, CSM 763, PLAT OF SURVEYS AND EXISTING CENTRELINE.

EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE FOR WASHBURN ROAD: GOVERNORS PLAT, DEERFIELD MEADOWS, PLAT OF SURVEYS AND EXISTING CENTRELINE.

EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE FOR MCGOVERN STREET: GOVERNORS PLAT, PLAT OF SURVEYS AND EXISTING CENTRELINE.

UTILITY NUMBER	OWNER(S)	INTEREST REQUIRED
201	ALLIANT ENERGY (ELECTRIC)	RELEASE OF RIGHTS
202	ALLIANT ENERGY (GAS)	RELEASE OF RIGHTS
203	FRONTIER COMMUNICATIONS OF WILC (COMMUNICATION)	RELEASE OF RIGHTS
204	VILLAGE OF DEERFIELD (ELECTRIC)	RELEASE OF RIGHTS
205	VILLAGE OF DEERFIELD (ELECTRIC)	RELEASE OF RIGHTS
206	CHARTER COMMUNICATIONS (COMMUNICATIONS)	RELEASE OF RIGHTS

- 201 ALLIANT ENERGY (ELECTRIC)
V 555 P 833 DOC 1420494 - PAR 1
NO RECORD EASEMENT - PAR 3
- 202 ALLIANT ENERGY (GAS)
V 403 P 115 DOC 1091727 - PAR 3
- 203 FRONTIER COMMUNICATIONS OF WILC (COMMUNICATIONS)
V 739 P 189 DOC 1251928 - PAR 1
- 204 VILLAGE OF DEERFIELD (SANITARY)
V 440 P 100 DOC 1365220 - PAR 2
- 205 VILLAGE OF DEERFIELD (ELECTRIC)
V 575 P 492 DOC 1427787 - PAR 3
- 206 CHARTER COMMUNICATIONS (COMMUNICATIONS)
NO RECORD EASEMENT - PAR 3

STATION & OFFSET TABLE

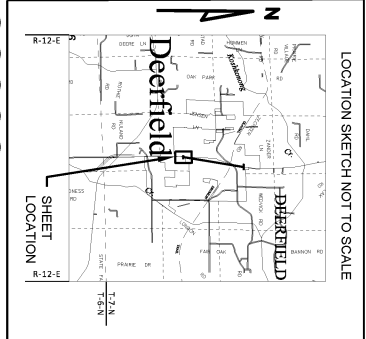
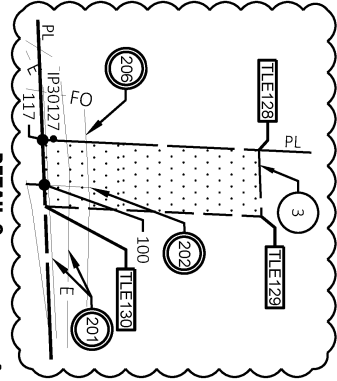
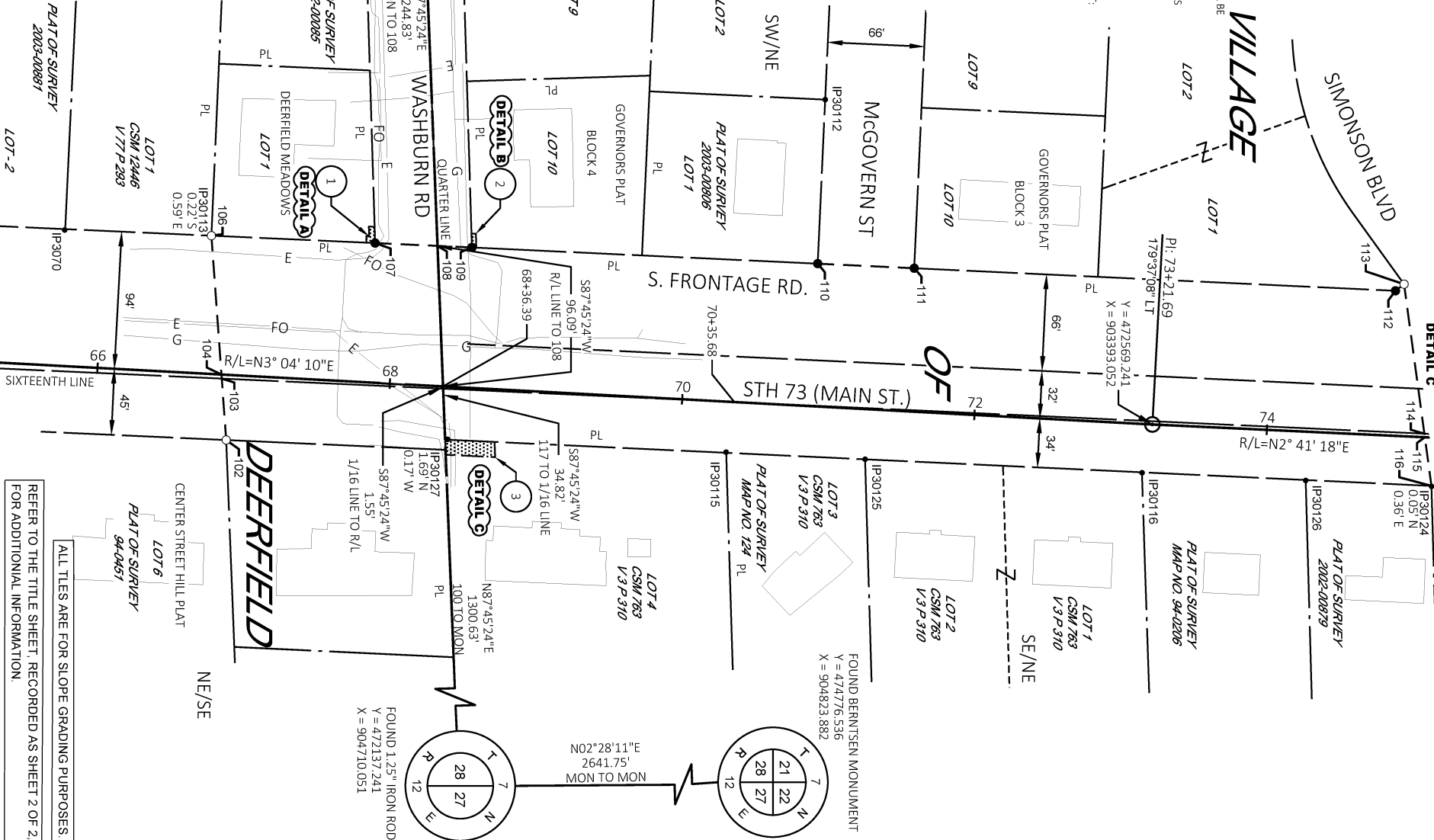
POINT NO.	STATION	OFFSET
100	68+40.40	43.20'
102	66+90.43	44.49'
103	66+85.15	2.75'
104	66+84.81	0.00'
106	66+72.88	94.36'
107	67+84.81	95.33'
108	68+27.49	95.68'
109	68+51.00	95.87'
110	70+87.51	97.44'
111	71+53.51	97.88'
112	74+83.30	99.00'
113	74+88.95	-103.81'
114	75+07.60	-2.41'
115	75+08.05	0.00'
116	75+14.12	33.00'
117	68+39.76	36.21'

COURSE TABLE

POINT NO.	STATION	BEARING	DISTANCE
100-102	67+88.86	S02°34'30"W	149.98'
102-103	67+88.49	S85°51'58"W	42.07'
103-104	67+80.50	S85°51'58"W	95.11'
104-106	67+29.94	N02°34'30"E	111.94'
106-107	67+483.88	N02°36'03"E	42.68'
107-108	68+50.21	N02°36'03"E	23.51'
108-109	68+54.01	N02°41'18"E	66.00'
109-110	68+73.58	N02°41'18"E	7.42'
110-111	68+74.55	N37°46'51"W	103.11'
111-112	68+74.55	N87°15'46"E	2.45'
112-113	68+74.55	N87°15'46"E	33.55'
113-114	68+74.55	S02°41'18"W	674.60'
114-115	68+74.55	N87°45'24"E	7.02'
115-116	68+74.55	N87°45'24"E	7.02'
116-117	68+74.55	N87°45'24"E	7.02'
117-100	68+74.55	N87°45'24"E	7.02'

COURSE TABLE

COURSE	BEARING	DISTANCE
100-102	S02°34'30"W	149.98'
102-103	S85°51'58"W	42.07'
103-104	S85°51'58"W	95.11'
104-106	N02°34'30"E	111.94'
106-107	N02°36'03"E	42.68'
107-108	N02°36'03"E	23.51'
108-109	N02°41'18"E	66.00'
109-110	N02°41'18"E	7.42'
110-111	N37°46'51"W	103.11'
111-112	N87°15'46"E	2.45'
112-113	N87°15'46"E	33.55'
113-114	S02°41'18"W	674.60'
114-115	N87°45'24"E	7.02'
115-116	N87°45'24"E	7.02'
116-117	N87°45'24"E	7.02'
117-100	N87°45'24"E	7.02'

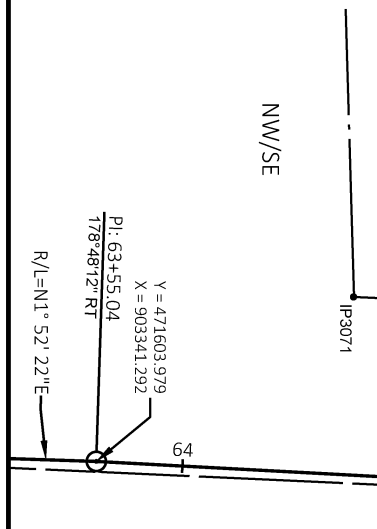
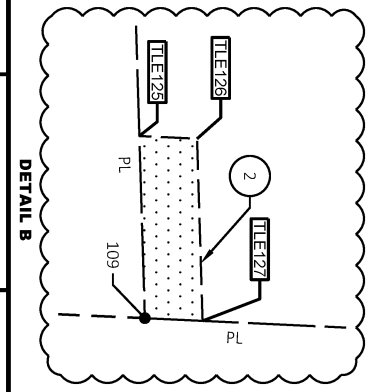
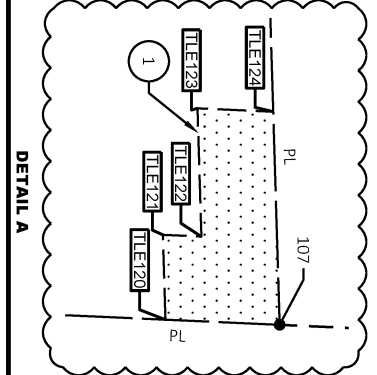


REGISTERED FOR REGISTER OF DEEDS
PROJECT NUMBER 3070-04-20-4.01
SHEET 1 OF 2
AMENDMENT NO. _____

KRISTE CHLEBOSKI
DANE COUNTY
REGISTER OF DEEDS

DOCUMENT #
5088205
01/21/2021 12:03 PM
This is a true and correct copy of the original document as submitted to the Register of Deeds.
Fees: 25.00
Pages: _____

SCALE, FEET 0 50 100



emcs LLC

ALL TILES ARE FOR SLOPE GRADING PURPOSES.
REFER TO THE TITLE SHEET RECORDED AS SHEET 2 OF 2, FOR ADDITIONAL INFORMATION.

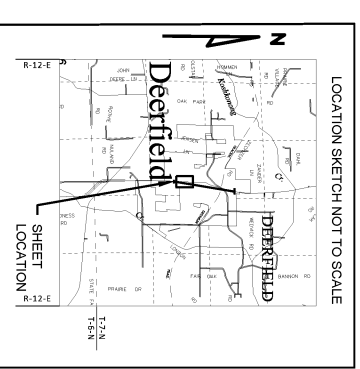
1, KEVIN C. BOVER, PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF THE DEPARTMENT OF TRANSPORTATION, I HAVE SURVEYED AND MAPPED THIS TRANSPORTATION PROJECT PLAT AND SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.

SIGNATURE: Kevin C. Bover DATE: 12/17/2020
PRINT NAME: KEVIN C. BOVER
REGISTRATION NUMBER: 5-2675

THIS PLAT AND RELOCATION ORDER ARE APPROVED FOR THE WISCONSIN DEPARTMENT OF TRANSPORTATION SOUTHWEST REGION - MADISON, WI
SIGNATURE: [Signature] DATE: 1/14/2021
PRINT NAME: Cody Schlegel

TRANSPORTATION PROJECT PLAT NO: 3070-04-20-4.02

BEING PART OF LOT 11, BLOCK 1 OF GOVERNORS PLAT AND PART OF OUTLOT 52 OF THE ASSESSOR'S AND CONSOLIDATED PLAT OF THE VILLAGE OF DEERFIELD LOCATED IN PART OF THE SW1/4 OF THE NE1/4, PART OF LOTS 1 AND 3, BLOCK 3 AND LOT 4, BLOCK 2 OF BUE'S ADDITION, LOCATED IN PART OF THE NW1/4 OF THE NE1/4, PART OF LOT 1 OF CSM 10810 AND PART OF LOTS 1 AND 6, BLOCK 2 OF VAN-ARNAM'S ADDITION LOCATED IN PART OF THE NE1/4 AND PART OF OUTLOTS 48 & 51 OF THE ASSESSOR'S AND CONSOLIDATED PLAT OF THE VILLAGE OF DEERFIELD AND PART OF LOT 1, BLOCK 1 OF SIMONSON'S ADDITION LOCATED IN PART OF THE SE1/4 OF THE NE1/4, ALL IN SECTION 28, TOWNSHIP 7 NORTH, RANGE 12 EAST, VILLAGE OF DEERFIELD, DANE COUNTY, WISCONSIN

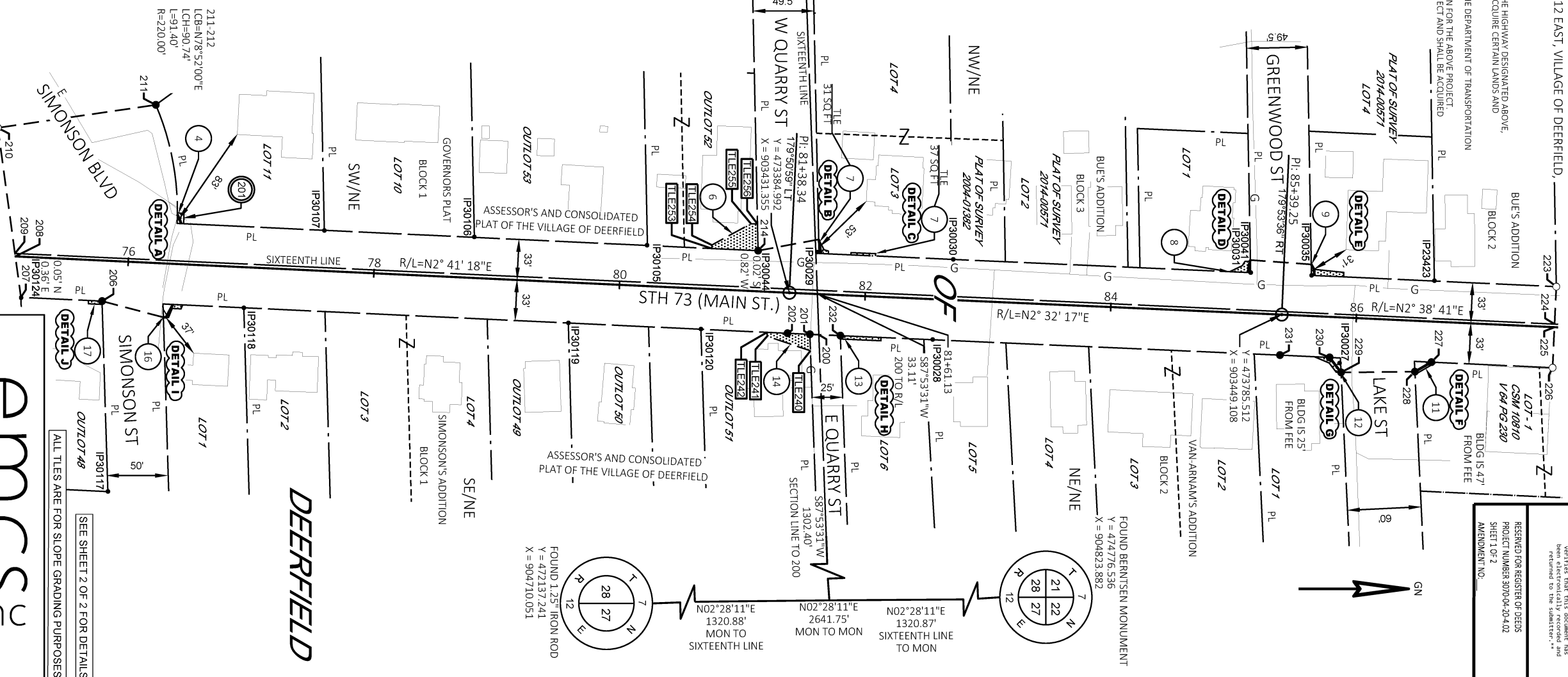


VILLAGE

NOTES: POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS). DANE COUNTY, MAD93 (2011) IN US SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES. FOR THE LATEST ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN MADISON, WI. ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 3/4"x24" IRON REBARS) UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT. EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE FOR STH73: GOVERNORS PLAT, ASSESSOR'S AND CONSOLIDATED PLAT OF THE VILLAGE OF DEERFIELD, BUE'S ADDITION, VAN-ARNAM'S ADDITION, SIMONSON'S ADDITION, CSM 10810, PLAT OF SURVEYS AND EXISTING CENTRELINE. EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE FOR SIMONSON'S BOUNDARY: GOVERNORS PLAT, AND EXISTING CENTRELINE. EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE FOR QUARRY STREET: ASSESSOR'S AND CONSOLIDATED PLAT OF THE VILLAGE OF DEERFIELD, BUE'S ADDITION, PLAT OF SURVEYS AND EXISTING CENTRELINE. EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE FOR GREENWOOD STREET: BUE'S ADDITION, PLAT OF SURVEYS AND EXISTING CENTRELINE. EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE FOR LAKE STREET: VAN-ARNAM'S ADDITION, CSM 10810, AND EXISTING CENTRELINE. EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE FOR EAST QUARRY STREET: ASSESSOR'S AND CONSOLIDATED PLAT OF THE VILLAGE OF DEERFIELD, VAN-ARNAM'S ADDITION, AND EXISTING CENTRELINE. EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE FOR SIMONSON STREET: SIMONSON'S ADDITION, ASSESSOR'S AND CONSOLIDATED PLAT OF THE VILLAGE OF DEERFIELD, AND EXISTING CENTRELINE.

Table with 3 columns: POINT, EXISTING MONUMENTS, DESCRIPTION. Lists various points (IP22423 to IP30124) and their corresponding monument types and descriptions.

Table with 3 columns: UTILITY NUMBER, UTILITY INTERESTS REQUIRED, INTEREST REQUIRED. Lists utility numbers 201 and 202, and their respective interests.

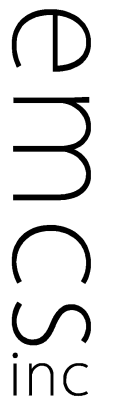


SCHEDULE OF LANDS & INTERESTS REQUIRED. Table with 6 columns: PARCEL NUMBER, OWNERS, INTEREST REQUIRED, R/W SQ. FT. REQUIRED, NEW EXISTING TOTAL SQ. FT., TILE. Lists parcels 4 through 17 and their details.

SCALE, FEET 0 50 100 REFER TO THE TITLE SHEET RECORDED AS SHEET 2 OF 2 IN TRANSPORTATION PROJECT PLATS AS DOCUMENT NO. 5988205 FOR ADDITIONAL INFORMATION.

KRISTI CHLERSKI DANE COUNTY REGISTER OF DEEDS DOCUMENT # 588853 01/27/2021 11:44 AM Trans Fee: Exempt #: Rec. Fee: 25.00

RESERVED FOR REGISTER OF DEEDS PROJECT NUMBER 3070-04-20-4.02 SHEET 1 OF 2 AMENDMENT NO.



1, KEVIN C. BOYER, PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF THE DEPARTMENT OF TRANSPORTATION, I HAVE SURVEYED AND MAPPED THIS TRANSPORTATION PROJECT PLAT AND SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND. SIGNATURE: Kevin C. Boyer DATE: 01/21/2021 PRINT NAME: KEVIN C. BOYER REGISTRATION NUMBER: S-2675 THIS PLAT AND RELOCATION ORDER ARE APPROVED FOR THE WISCONSIN DEPARTMENT OF TRANSPORTATION SOUTHWEST REGION - MADISON SIGNATURE: Greg Schilling DATE: 1/25/2021

TRANSPORTATION PROJECT PLAT NO: 3070-04-20-4.02

RELOCATION ORDER 5TH/73, 139 TO COLUMBUS, SHAUL LANE TO NORTH STREET, DANE COUNTY

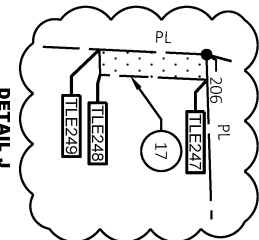
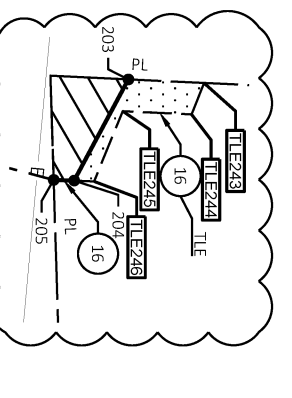
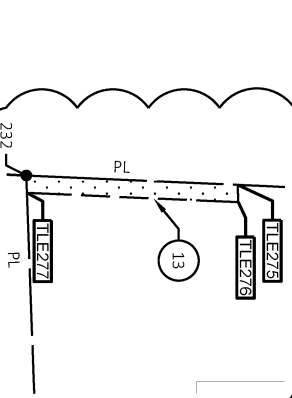
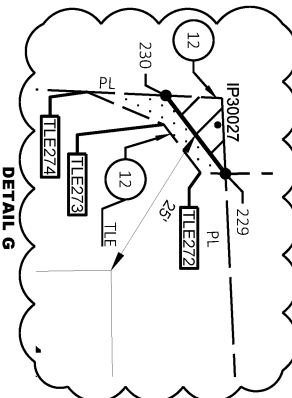
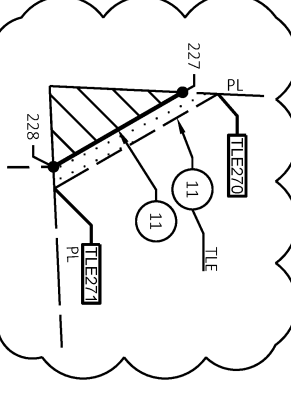
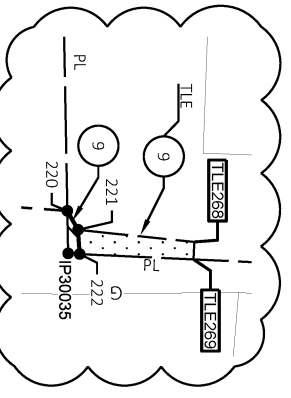
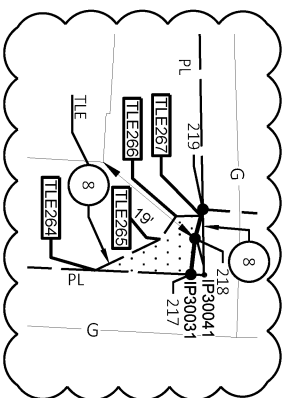
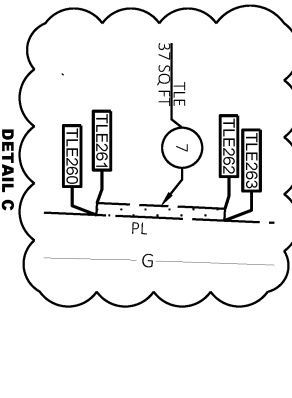
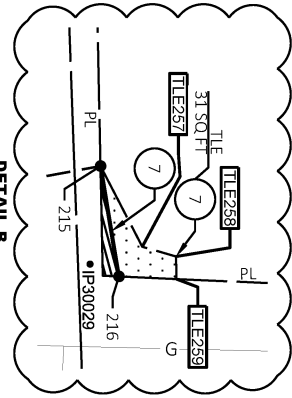
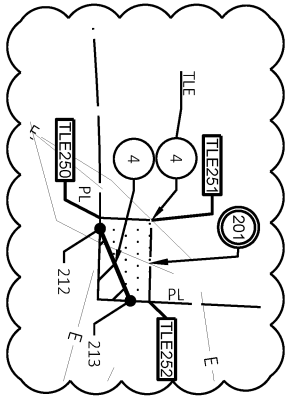
POINT NO.	STATION	OFFSET
TLE240	81+57.55	46.37'
TLE241	81+21.97	36.20'
TLE242	81+21.69	33.00'
TLE243	76+45.81	33.00'
TLE244	76+44.66	36.33'
TLE245	76+37.49	36.33'
TLE246	76+34.86	43.78'
TLE247	76+80.36	35.62'
TLE248	75+69.20	35.63'
TLE249	75+69.00	33.00'
TLE250	76+38.07	-41.50'
TLE251	76+43.43	-41.51'
TLE252	76+43.60	-33.00'
TLE253	80+73.72	33.00'
TLE254	80+73.52	-35.85'
TLE255	81+04.56	-54.68'
TLE256	81+09.82	-54.68'
TLE257	81+64.97	-35.34'
TLE258	81+68.67	33.00'
TLE259	81+67.08	-33.00'
TLE260	81+67.08	-34.87'
TLE261	81+67.08	-34.86'
TLE262	82+07.08	-33.00'
TLE263	82+07.08	-33.00'
TLE264	84+95.40	33.00'
TLE265	85+05.35	38.30'
TLE266	85+07.65	42.01'
TLE267	85+11.54	-42.26'
TLE268	85+88.07	-36.63'
TLE269	85+88.07	-33.00'
TLE270	86+46.34	33.00'
TLE271	86+49.77	43.63'
TLE272	85+85.70	44.78'
TLE273	85+79.64	37.55'
TLE274	85+67.39	33.00'
TLE275	82+14.35	33.00'
TLE276	82+14.57	35.69'
TLE277	81+81.52	35.69'

POINT NO.	STATION	OFFSET
200	81+63.82	33.00'
201	81+56.47	33.00'
202	81+38.34	33.00'
203	76+37.93	33.00'
204	76+32.78	43.80'
205	76+30.65	43.82'
206	75+80.16	33.00'
207	75+14.12	33.00'
208	75+08.05	0.00'
209	75+07.60	-2.41'
210	74+88.95	-103.81'
211	76+16.43	-128.49'
212	76+38.11	-40.37'
213	76+41.61	-33.00'
214	81+11.29	-33.00'
215	81+60.25	-44.40'
216	81+62.66	-33.00'
217	85+10.33	-33.00'
218	85+10.73	-38.64'
219	85+11.76	-43.25'
220	85+61.37	-41.87'
221	85+63.77	-38.04'
222	85+64.35	-33.00'
223	87+61.29	-33.00'
224	87+61.29	-2.54'
225	87+61.29	0.00'
226	87+61.29	33.00'
227	86+62.64	33.00'
228	86+49.56	41.39'
229	85+89.62	44.79'
230	85+79.75	33.00'
231	85+39.29	33.00'
232	81+81.30	33.00'

COURSE	BEARING	DISTANCE
200-201	S02°32'17"W	7.35'
201-202	S02°32'17"W	18.17'
202-203	S02°41'18"W	500.46'
203-204	S61°49'00"E	11.96'
204-205	S02°01'55"W	2.13'
205-206	S14°47'16"W	51.64'
206-207	S02°41'18"W	66.04'
207-208	S82°15'46"W	33.55'
208-209	S82°15'46"W	2.45'
209-210	S82°15'46"W	103.11'
210-211	N08°15'52"W	129.85'
211-212	SEE CURVE DATA	
212-213	N67°18'27"E	8.16'
213-214	N02°41'18"E	469.69'
214-215	N10°34'04"W	50.16'
215-216	N80°34'04"W	11.66'
216-217	N02°32'17"E	347.67'
217-218	N83°26'04"W	5.65'
218-219	N74°53'56"W	4.73'
219-220	N04°10'34"E	49.71'
220-221	N60°31'14"E	4.53'
221-222	N86°08'48"E	5.07'
222-223	N02°38'41"E	196.95'
223-224	S87°21'19"E	30.47'
224-225	S87°21'19"E	2.53'
225-226	S87°21'19"E	33.00'
226-227	S02°38'41"W	98.66'
227-228	S30°02'16"E	15.53'
228-229	S00°36'11"E	60.04'
229-230	S52°41'59"W	15.38'
230-231	S02°38'41"W	40.47'
231-232	S02°32'17"W	357.93'
232-200	S02°32'17"W	17.48'



SHEET IS NOT TO SCALE



REFER TO THE TITLE SHEET, RECORDED AS SHEET 2 OF 2, IN TRANSPORTATION PROJECT PLATS AS DOCUMENT NO. 5686205 FOR ADDITIONAL INFORMATION.

TPP NUMBER: 3070-04-20-4.02
SHEET 2 OF 2

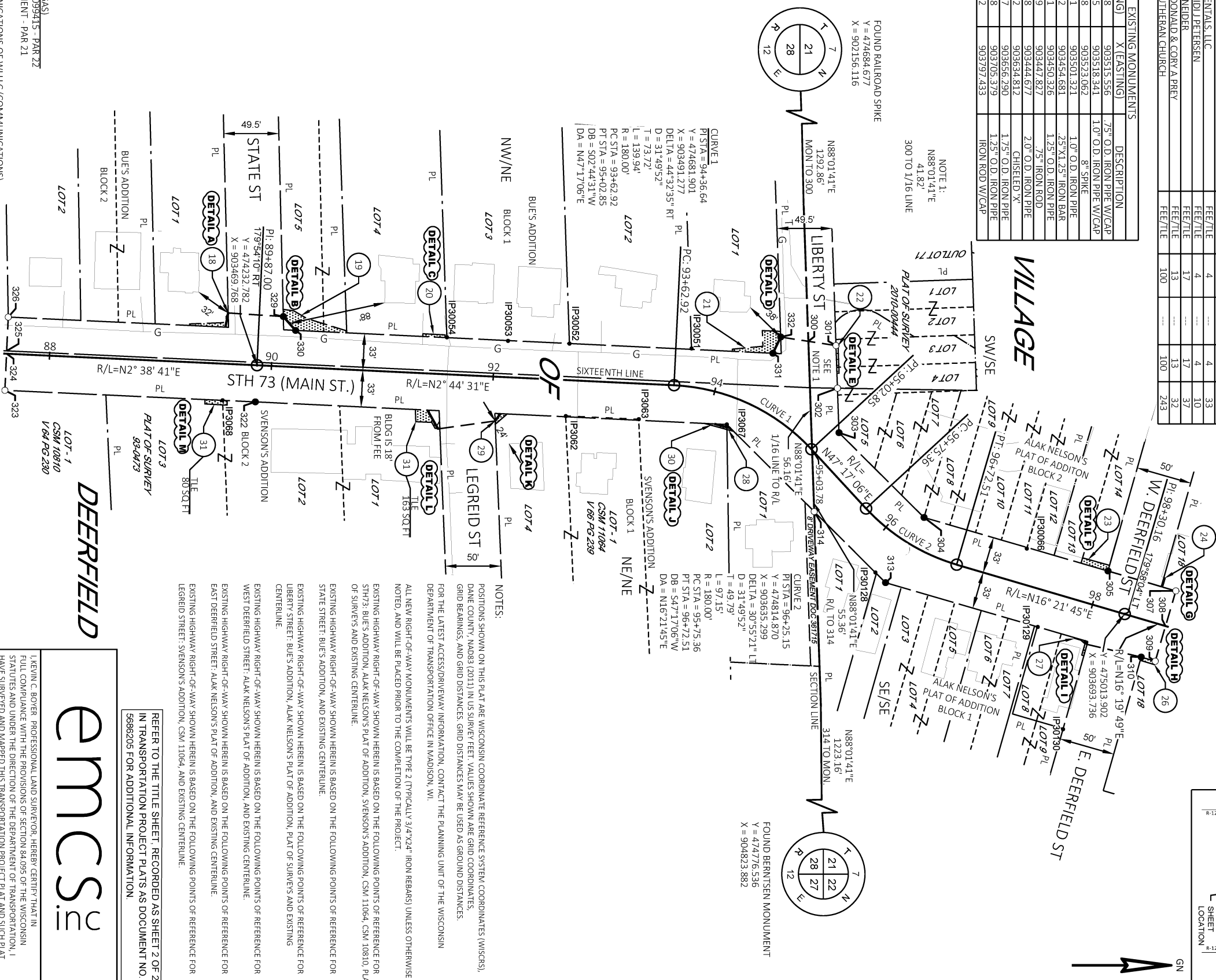
TRANSPORTATION PROJECT PLAT NO: 3070-04-20-4.03

BEING PART OF LOT 1, BLOCK 2 AND PART OF LOTS 1, 4 AND 5 OF BLOCK 1 OF BUE'S ADDITION LOCATED IN PART OF THE NW1/4 OF THE NE1/4 AND PART OF LOTS 1, 2 AND 4, BLOCK 1 AND PART OF LOT 1, 2 AND 3, BLOCK 2 OF SVENSON'S ADDITION LOCATED IN PART OF THE NE1/4 OF THE NE1/4 OF SECTION 28, ALSO PART OF LOTS 3, 4 BLOCK 2 LOCATED IN ALAK NELSON'S PLAT OF ADDITION LOCATED IN PART OF THE SW1/4 OF THE SE1/4 AND PART OF LOT 14 AND LOT 18, BLOCK 2 OF ALAK NELSON'S PLAT OF ADDITION AND PART OF LOT 9 AND 18 BLOCK 1 OF ALAK NELSON'S PLAT OF ADDITION LOCATED IN PART OF THE SE1/4 OF THE SE1/4 OF SECTION 21, ALL IN TOWNSHIP 7 NORTH, RANGE 12 EAST, VILLAGE OF DEERFIELD, DANE COUNTY, WISCONSIN

RELOCATION ORDER - STH 73, 139 TO COLUMBUS, SHAUL LANE TO NORTH STREET, DANE COUNTY TO PROPERTY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE. THE DEPARTMENT OF TRANSPORTATION DEEMST NECESSARY TO RELOCATE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE PROJECT.

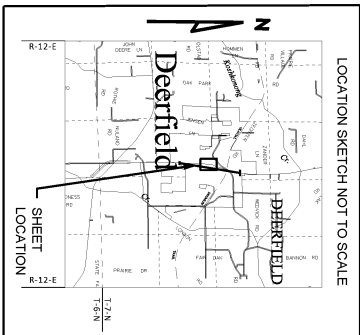
Table with columns: PARCEL NUMBER, OWNERS, INTEREST, R/W, SQ. FT., REQUIRED, NEW, EXISTING, TOTAL, SQ. FT., TILE. Lists parcels 18-31 and their respective owners and interests.

Table with columns: POINT, Y (NORTHING), X (EASTING), DESCRIPTION. Lists existing monuments like iron pipes and spikes with their coordinates and descriptions.



SCALE, FEET 0 50 100

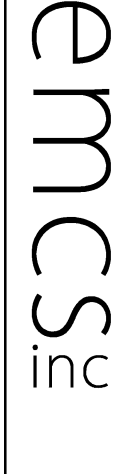
Administrative information including document number (5888335), date (01/27/2021), and recording details.



NOTES: POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), DANE COUNTY, NAD83 (2011) IN US SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES.

EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE FOR STATE STREET: BUE'S ADDITION, ALAK NELSON'S PLAT OF ADDITION, PLAT OF SURVEYS AND EXISTING CENTRELINE.

REFER TO THE TITLE SHEET, RECORDED AS SHEET 2 OF 2, IN TRANSPORTATION PROJECT PLATS AS DOCUMENT NO. 5888205 FOR ADDITIONAL INFORMATION.



Signature and registration information for Kevin C. Boyer, Wisconsin Professional Land Surveyor, including registration number 52675 and date 01/21/2021.

- 202 ALLIANT ENERGY (GAS) V 407 P 525 DOC 1059415 - PAR 22 NO RECORD EASEMENT - PAR 21
- 203 FRONTIER COMMUNICATIONS OF WILLC (COMMUNICATIONS) V 130 P 186 DOC 1249280 - PAR 22

UTILITY INTERESTS REQUIRED table with columns: UTILITY, INTEREST, OWNER(S), REQUIRED, RELEASE OF RIGHTS.

ALL TILES ARE FOR SLOPE GRADING PURPOSES.

SEE SHEET 2 OF 2 FOR DETAILS.

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), DANE COUNTY, NAD83 (2011) IN US SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

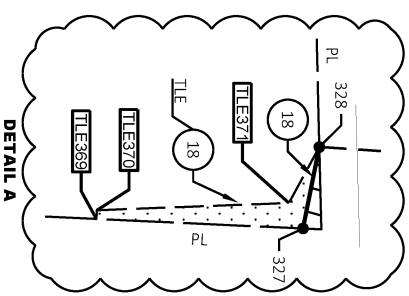
TRANSPORTATION PROJECT PLAT NO : 3070-04-20-4.03

RELOCATION ORDER 5TH73.139 TO COLUMBUS, SHAUL LANE TO NORTH STREET, DANE COUNTY

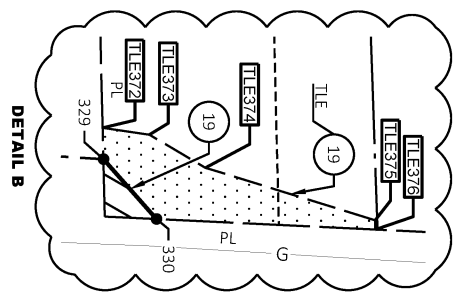
STATION & OFFSET TABLE	STATION	OFFSET
300	94+50.31	-74.72'
301	94+66.22	-85.10'
302	94+84.11	-55.86'
303	95+08.76	-28.38'
304	96+24.70	-27.20'
305	98+04.88	-37.94'
306	98+54.90	-33.00'
307	98+68.56	-33.00'
308	98+68.56	0.00'
309	98+68.56	33.00'
310	98+55.42	33.00'
311	98+05.46	35.70'
312	98+02.17	33.06'
313	96+25.69	40.91'
314	95+45.72	36.13'
315	94+26.04	26.85'
316	94+08.61	28.13'
317	92+08.03	33.00'
318	92+03.55	39.83'
319	91+54.18	49.08'
320	91+52.31	49.09'
321	91+42.24	33.00'
322	89+66.97	33.00'
323	87+61.29	33.00'
324	87+61.29	0.00'
325	87+61.29	-2.54'
326	87+61.29	-33.00'
327	89+56.97	-33.00'
328	89+58.94	-43.83'
329	90+08.56	-44.92'
330	90+20.19	-33.00'
331	94+33.10	-50.29'
332	94+33.20	-66.49'

COURSE TABLE	COURSE	BEARING	DISTANCE
300-301	N08°46'55"E	25.19'	
301-302	N88°01'41"E	38.38'	
302-303	N88°01'41"E	40.15'	
303-304	N44°53'02"E	108.05'	
304-305	N16°14'07"E	172.48'	
305-306	N16°16'56"E	50.00'	
306-307	N16°19'49"E	13.67'	
307-308	S73°40'11"E	33.00'	
308-309	S73°40'11"E	33.00'	
309-310	S16°19'49"W	13.14'	
310-311	S13°15'05"W	50.01'	
311-312	S55°05'45"W	4.22'	
312-313	S16°14'07"W	186.48'	
313-314	S44°53'02"W	906.68'	
314-315	S44°53'02"W	106.36'	
315-316	S15°06'20"W	14.81'	
316-317	S02°44'31"W	193.02'	
317-318	S54°00'13"E	8.17'	
318-319	S07°52'11"E	50.23'	
319-320	S02°34'39"W	1.87'	
320-321	S60°41'10"W	18.98'	
321-322	S02°44'31"W	155.21'	
322-323	S02°38'41"W	225.68'	
323-324	N87°21'19"W	33.00'	
324-325	N87°21'19"W	2.53'	
325-326	N87°21'19"W	30.47'	
326-327	N02°38'41"E	195.68'	
327-328	N78°36'01"W	12.98'	
328-329	N03°44'08"E	49.70'	
329-330	N48°27'17"E	16.66'	
330-331	N02°44'31"E	430.26'	
331-332	N64°26'50"W	16.20'	
332-330	N08°46'55"E	25.19'	

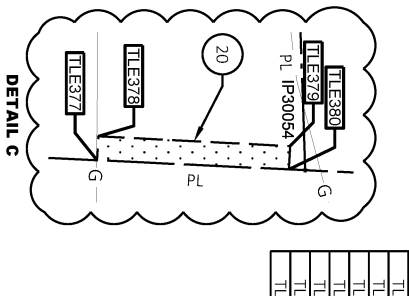
TILE STATION & OFFSET TABLE	POINT NO.	STATION	OFFSET
TILE340	94+67.22	86.25'	
TILE341	94+78.43	67.01'	
TILE342	94+77.39	-65.77'	
TILE343	97+79.62	-37.89'	
TILE344	97+79.61	-37.86'	
TILE345	98+04.83	-38.29'	
TILE346	98+54.83	-41.49'	
TILE347	98+56.09	-41.33'	
TILE348	98+56.14	-33.00'	
TILE349	98+68.64	36.08'	
TILE350	98+55.44	36.03'	
TILE351	98+05.55	46.29'	
TILE352	98+03.55	46.29'	
TILE353	98+03.46	35.72'	
TILE354	97+98.99	33.65'	
TILE355	97+92.81	33.66'	
TILE356	97+92.80	33.08'	
TILE357	94+30.47	28.32'	
TILE358	93+97.74	30.21'	
TILE359	92+03.81	33.00'	
TILE360	92+14.11	42.72'	
TILE361	91+49.95	49.10'	
TILE362	91+46.61	43.84'	
TILE363	91+32.23	43.88'	
TILE364	91+31.77	33.00'	
TILE365	89+61.76	37.06'	
TILE366	89+41.67	36.92'	
TILE367	89+41.70	33.00'	
TILE368	89+24.68	-33.00'	
TILE369	89+24.68	-34.37'	
TILE370	89+24.68	-34.37'	
TILE371	89+54.61	-36.89'	
TILE372	90+08.09	-51.45'	
TILE373	90+17.87	-50.46'	
TILE374	90+29.87	-44.16'	
TILE375	90+65.69	-34.93'	
TILE376	90+65.84	-33.00'	
TILE377	91+34.88	-33.00'	
TILE378	91+34.89	-35.49'	
TILE379	91+54.90	-35.42'	
TILE380	91+54.88	-33.00'	
TILE381	94+05.22	39.07'	
TILE382	94+04.71	40.93'	
TILE383	94+24.69	-52.41'	
TILE384	94+20.41	-65.38'	
TILE385	94+31.54	-71.02'	



DETAIL A

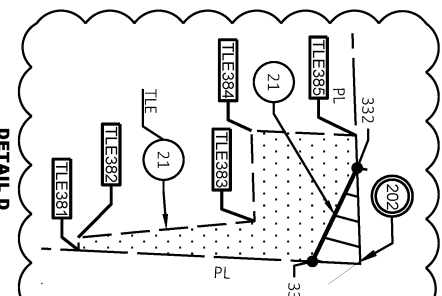
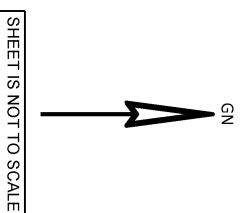


DETAIL B

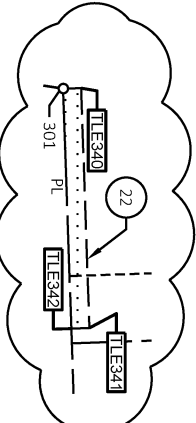


DETAIL C

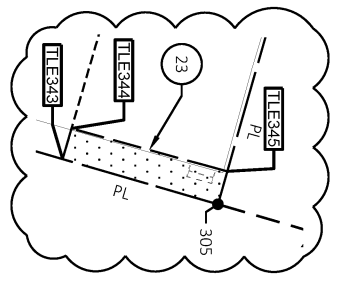
TILE379	91+49.95	49.10'
TILE380	91+32.23	43.88'
TILE381	91+31.77	33.00'
TILE382	89+61.76	37.06'
TILE383	89+41.67	36.92'
TILE384	89+41.70	33.00'
TILE385	89+24.68	-33.00'
TILE386	89+24.68	-34.37'
TILE387	89+24.68	-34.37'
TILE388	89+54.61	-36.89'
TILE389	90+08.09	-51.45'
TILE390	90+17.87	-50.46'
TILE391	90+29.87	-44.16'
TILE392	90+65.69	-34.93'
TILE393	90+65.84	-33.00'
TILE394	91+34.88	-33.00'
TILE395	91+34.89	-35.49'
TILE396	91+54.90	-35.42'
TILE397	91+54.88	-33.00'
TILE398	94+05.22	39.07'
TILE399	94+04.71	40.93'
TILE400	94+24.69	-52.41'
TILE401	94+20.41	-65.38'
TILE402	94+31.54	-71.02'



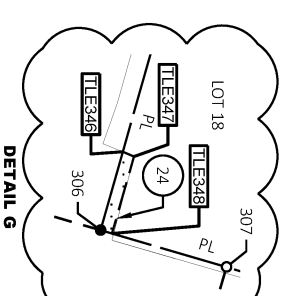
DETAIL D



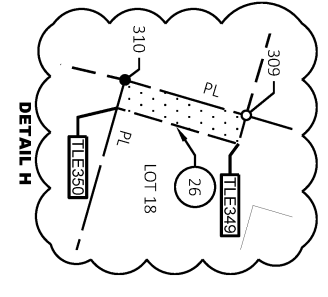
DETAIL E



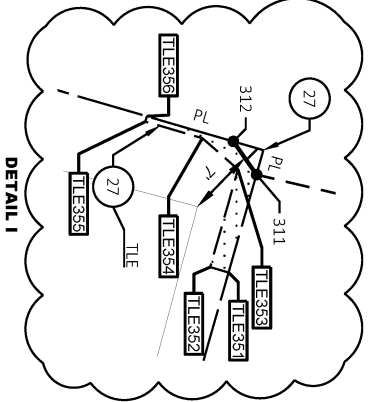
DETAIL F



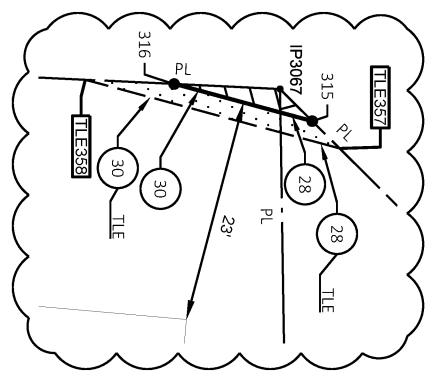
DETAIL G



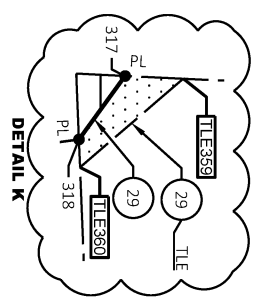
DETAIL H



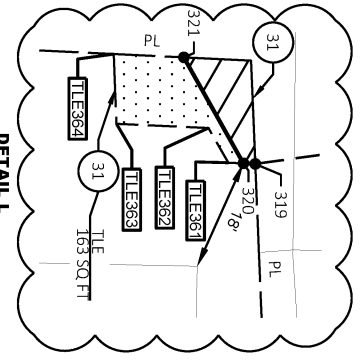
DETAIL I



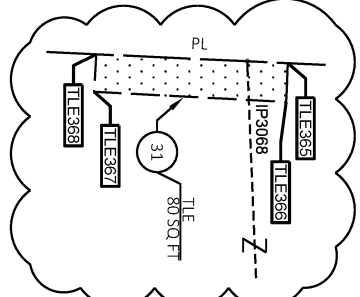
DETAIL J



DETAIL K



DETAIL L



DETAIL M

REFER TO THE TITLE SHEET, RECORDED AS SHEET 2 OF 2, IN TRANSPORTATION PROJECT PLATS AS DOCUMENT NO. 5686205 FOR ADDITIONAL INFORMATION.

TPP NUMBER: 3070-04-20-4-03 SHEET 2 OF 2

TRANSPORTATION PROJECT PLAT NO: 3070-04-20-4.04

BEING PART OF LOT 11, BLOCK 4, PART OF LOT 1 AND LOT 11, BLOCK 5, AND PART OF LOT 1, BLOCK 8 OF ALAK NELSON'S PLAT OF ADDITION, LOCATED IN PART OF THE SE1/4 OF THE SE1/4, AND PART OF THE SE1/4 OF THE SE1/4, ALL IN SECTION 21, TOWNSHIP 7 NORTH, RANGE 12 EAST, VILLAGE OF DEERFIELD, DANE COUNTY, WISCONSIN

RELOCATION ORDER - STH 73, 139 TO COLUMBUS, SHAUL LANE TO NORTH STREET, DANE COUNTY
 TO PROPERTY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE. THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMST NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE PROJECT.
 TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SECTION 84.02 (2), 84.09 AND 84.30, WISCONSIN STATUTES, THE DEPARTMENT OF TRANSPORTATION HEREBY ORDERS THAT:
 1. THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS Laid OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE PROJECT.
 2. THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE DEPARTMENT FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE STATE OF WISCONSIN, PURSUANT TO THE PROVISIONS OF SECTION 84.09 (1) OR (2), WISCONSIN STATUTES.

SCHEDULE OF LANDS & INTERESTS REQUIRED

PARCEL NUMBER	OWNERS	INTEREST	R/W SQ. FT. REQUIRED	TILE NUMBER
32	W/G ENTERPRISES, LLC	NEW	4	157
33	W/DNR	FEE/TILE	213	272
34	VILLAGE OF DEERFIELD	FEE/TILE	213	125
36	DEERFIELD TIRE AND AUTO, LLC	FEE/TILE	16	126
37	ALAN T. MIKKESEN	FEE	4	4
38	DEERFIELD RENTALS, LLC	TILE	---	---

POINT	Y (NORTHING)	X (EASTING)	DESCRIPTION
IP30062	475524.254	903876.881	CHISELED 'X'
IP30063	475484.209	903797.146	CHISELED 'X'
IP30064	475421.455	903797.146	7.5" IRON ROD
IP30065	475118.215	903690.534	1.75" O.D. IRON PIPE
IP30073	475598.784	903898.018	IRON ROD W/CAP
IP30074	475625.632	903906.364	7.5" IRON ROD
IP30131	476026.351	904023.991	1.0" O.D. IRON PIPE
IP30132	475921.471	903992.039	1.0" IRON ROD BENT
IP30133	475896.649	904107.539	1.50" O.D. IRON PIPE

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCS). DANE COUNTY, MAD83 (2011) IN US SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES FOR THE LATEST ACCESS/DRIVEWAY INFORMATION. CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN MADISON, WI.

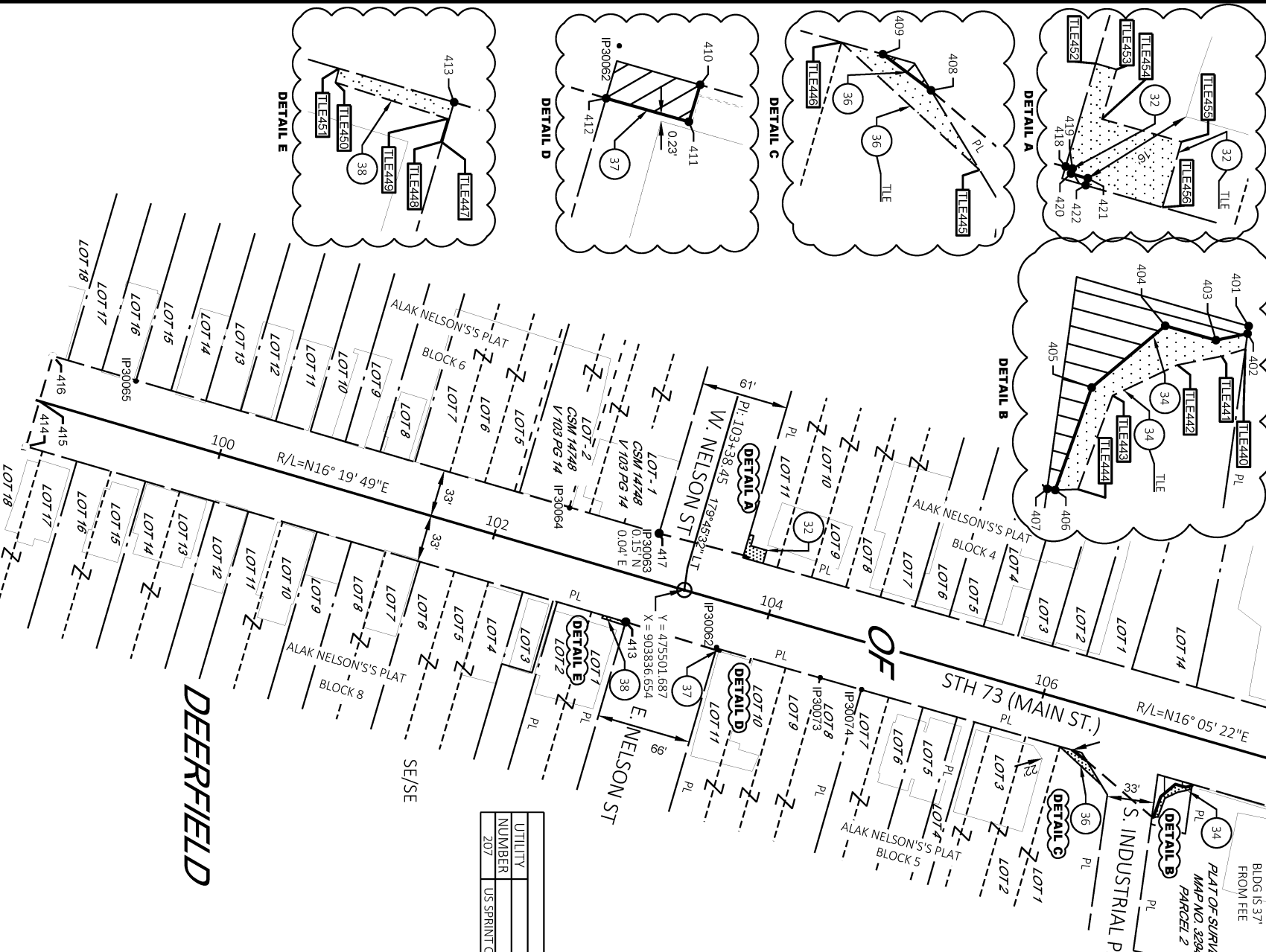
ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 3/4"x24" IRON REBAR) UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE FOR STH73: ALAK NELSON'S PLAT, CSM 14748, CSM 5578, PLAT OF SURVEYS AND EXISTING CENTERLINE.

EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE FOR WEST NELSON STREET: ALAK NELSON'S PLAT, CSM 14748, AND EXISTING CENTERLINE.

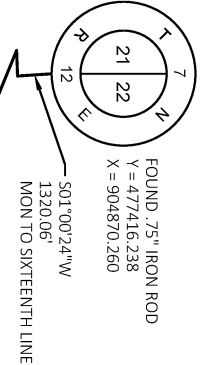
EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE FOR SOUTH INDUSTRIAL PARK ROAD: ALAK NELSON'S PLAT, PLAT OF SURVEYS AND EXISTING CENTERLINE.

EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE FOR EAST NELSON STREET: ALAK NELSON'S PLAT, AND EXISTING CENTERLINE.



TILE STATION & OFFSET TABLE
POINT NO STATION OFFSET
TILE40 107+17.83 35.43'
TILE41 107+14.93 37.29'
TILE42 107+09.31 40.18'
TILE43 107+01.80 46.81'
TILE44 107+00.78 60.74'
TILE45 106+56.36 50.10'
TILE46 106+21.14 32.40'
TILE47 103+05.49 38.87'
TILE48 103+05.24 38.87'
TILE49 103+05.29 35.48'
TILE50 102+88.80 35.00'
TILE51 102+88.80 33.00'
TILE52 103+71.60 49.84'
TILE53 103+74.63 49.85'
TILE54 103+74.59 42.62'
TILE55 103+86.17 42.65'
TILE56 103+86.14 33.10'
TILE57 108+29.13 34.04'
TILE58 108+29.15 45.57'
TILE59 108+52.71 45.62'
TILE60 108+52.69 34.09'

VILLAGE



STATION & OFFSET TABLE
POINT NO. STATION OFFSET
400 109+37.94 31.73'
401 107+17.37 32.20'
402 107+17.52 33.25'
403 107+13.53 35.36'
404 107+06.15 35.38'
405 106+98.67 46.55'
406 106+97.75 61.75'
407 106+96.63 61.90'
408 106+41.97 36.89'
409 106+30.12 32.38'
410 103+74.37 32.92'
411 103+74.35 34.27'
412 103+71.34 34.29'
413 103+05.49 33.00'
414 98+68.56 33.00'
415 98+68.56 0.00'
416 98+68.56 33.00'
417 103+10.41 33.00'
418 103+71.55 34.81'
419 103+72.55 34.81'
420 103+72.55 34.07'
421 103+74.98 34.08'
422 103+74.98 33.08'
423 109+16.48 34.23'
424 109+27.62 0.00'

COURSE TABLE
COURSE BEARING DISTANCE
400-401 S15° 58' 04" W 220.57'
401-402 S82° 05' 36" E 1.06'
402-403 S11° 51' 22" E 4.51'
403-404 S15° 55' 41" W 7.39'
404-405 S40° 05' 25" E 13.44'
405-406 S70° 27' 26" E 15.23'
406-407 S08° 15' 06" W 1.13'
407-408 S40° 40' 52" W 60.11'
408-409 S36° 54' 40" W 12.68'
409-410 S15° 58' 04" W 255.75'
410-411 S73° 00' 01" E 1.35'
411-412 S15° 48' 46" W 3.01'
412-413 S17° 19' 38" W 66.01'
413-414 S16° 19' 49" W 436.92'
414-415 N73° 40' 11" W 33.00'
415-416 N73° 40' 11" W 33.00'
416-417 N16° 19' 49" E 441.85'
417-418 N14° 30' 02" E 61.03'
418-419 N15° 58' 04" E 1.00'
419-420 S73° 44' 11" E 0.74'
420-421 N15° 58' 04" E 2.43'
421-422 S73° 44' 11" E 1.00'
422-423 N15° 58' 04" E 541.50'
423-424 N88° 03' 57" E 35.99'
424-400 N88° 03' 57" E 33.37'

UTILITY INTERESTS REQUIRED
UTILITY NUMBER INTEREST REQUIRED
207 US SPRINT COMMUNICATIONS COMPANY (COMMUNICATION) RELEASE OF RIGHTS

ALL TILES ARE FOR SLOPE GRADING PURPOSES.
 REFER TO THE TITLE SHEET, RECORDED AS SHEET 2 OF 2, IN TRANSPORTATION PROJECT PLATS AS DOCUMENT NO. 5698205 FOR ADDITIONAL INFORMATION.



1. KEVIN C. BOYER, PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF THE DEPARTMENT OF TRANSPORTATION, I HAVE SURVEYED AND MAPPED THIS TRANSPORTATION PROJECT PLAT AND SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.

DATE: 01/26/2021

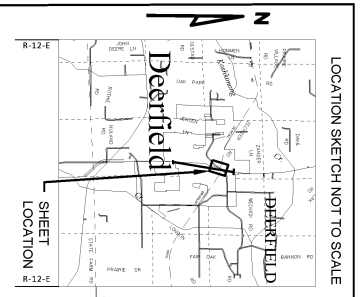
PRINT NAME: KEVIN C. BOYER

REGISTRATION NUMBER: S-2675

THIS PLAT AND RELOCATION ORDER ARE APPROVED FOR THE WISCONSIN DEPARTMENT OF TRANSPORTATION SOUTHWEST REGION - MADISON

DATE: 1/26/2021

PRINT NAME: *Kevin C. Boyer*



RESERVED FOR REGISTER OF DEEDS
 AMENDMENT NO. _____

DOCUMENT # 5698205
 61/27/2021 11:44 AM
 Trans Fee: \$25.00
 Rec. Fee: \$25.00
 Pages: 4
 The above recording information heretofore that this document has returned to the submitter.

TRANSPORTATION PROJECT PLAT NO: 3070-04-20-4.04 - AMENDMENT NO: 1

SCALE, FEET 0 50 100

AMENDMENT ADDS PARCEL 107 AND REMOVES UTILITY NUMBER 207 OF TRANSPORTATION PROJECT PLAT 3070-04-20-4.04 RECORDED AS DOCUMENT NUMBER 5688356

BEING PART OF LOT 11, BLOCK 4, PART OF LOT 1 AND LOT 11, BLOCK 5, AND PART OF LOT 1, BLOCK 8 OF ALAK NELSON'S PLAT OF ADDITION, LOCATED IN PART OF THE SE1/4 OF THE SE1/4, AND PART OF THE SE1/4 OF THE SE1/4, ALL IN SECTION 21, TOWNSHIP 7 NORTH, RANGE 12 EAST, VILLAGE OF DEERFIELD, DANE COUNTY, WISCONSIN

RELOCATION ORDER - STH 73, 139 TO COLUMBUS, SHAUL LANE TO NORTH STREET, DANE COUNTY

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMST NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS ON RIGHTS IN LANDS FOR THE ABOVE PROJECT.

HEREBY ORDERS THAT: PURSUANT TO AUTHORITY GRANTED UNDER SECTION 84.02 (1), 84.09, AND 84.30, WISCONSIN STATUTES, THE DEPARTMENT OF TRANSPORTATION

1. THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS Laid OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE PROJECT.

2. THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE DEPARTMENT FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE STATE OF WISCONSIN, PURSUANT TO THE PROVISIONS OF SECTION 84.09 (1) OR (2), WISCONSIN STATUTES.

SCHEDULE OF LANDS & INTERESTS REQUIRED

Table with columns: PARCEL NUMBER, OWNERS, INTEREST, R/W SQ. FT. REQUIRED, NEW EXISTING TOTAL, SQ. FT. TILE

NOTES:

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCS). DANE COUNTY, MAD83 (2011) IN US SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

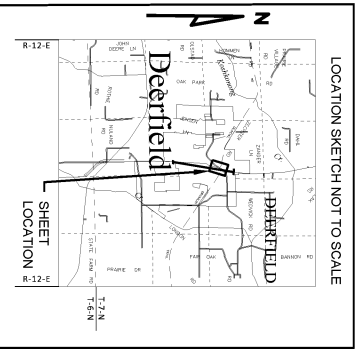
ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 3/4"x24" IRON REBAR) UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE FOR STH73: ALAK NELSON'S PLAT, CSM 14748, CSM 5578, PLAT OF SURVEYS AND EXISTING CENTRELINE.

EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE FOR WEST NELSON STREET: ALAK NELSON'S PLAT, CSM 14748, AND EXISTING CENTRELINE.

EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE FOR SOUTH INDUSTRIAL PARK ROAD: ALAK NELSON'S PLAT, PLAT OF SURVEYS AND EXISTING CENTRELINE.

EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE FOR EAST NELSON STREET: ALAK NELSON'S PLAT, AND EXISTING CENTRELINE.



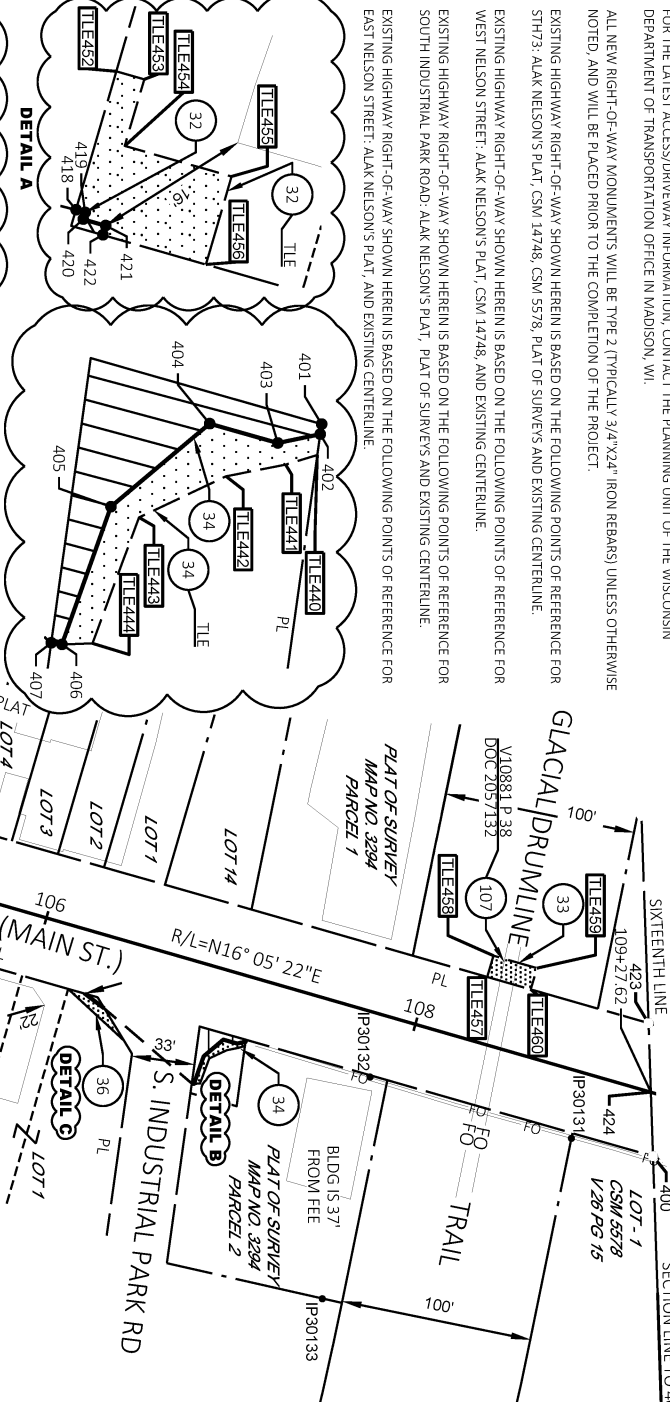
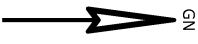
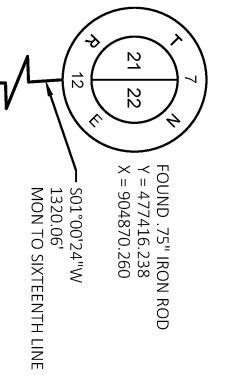
REGISTERED FOR REGISTER OF DEEDS PROJECT NUMBER 3070-04-20-4.04 AMENDMENT NO. 1

KRISTI CHLEBOWSKI DANE COUNTY REGISTER OF DEEDS DOCUMENT # 5686771 10/07/2022 11:56 AM

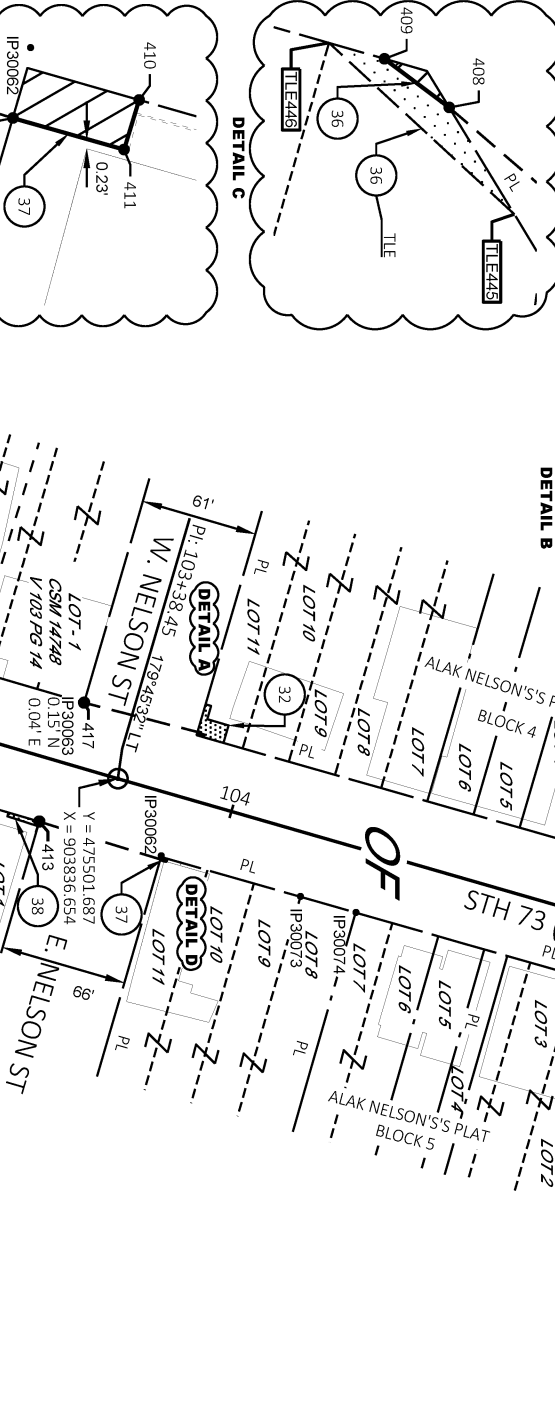
Trans Fee: Exempt #: Rec. Fee: 25.00

Pages: This document recording information has been electronically recorded and returned to the submitter.

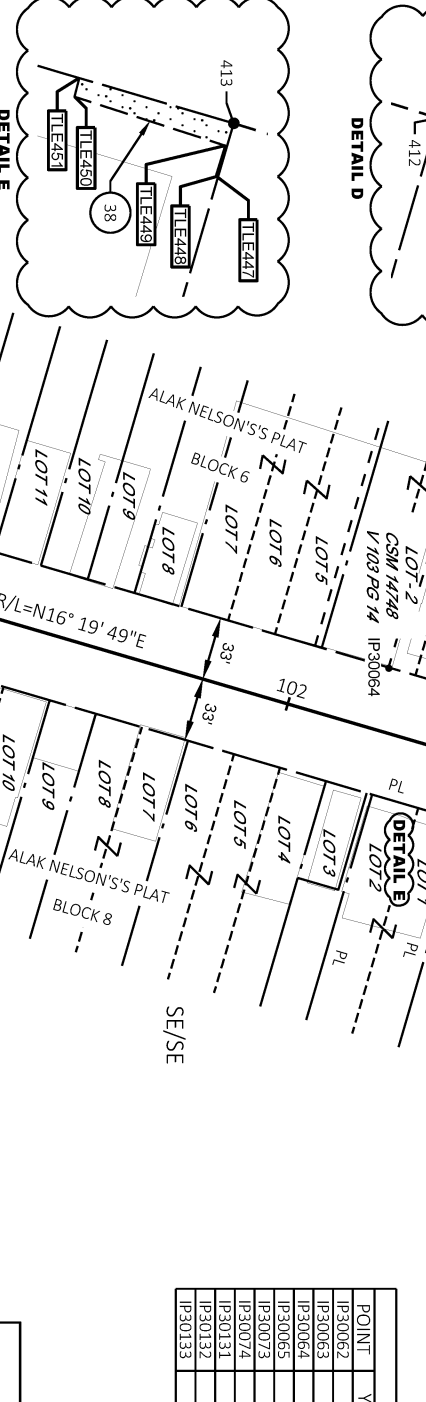
TILE STATION & OFFSET TABLE with columns: POINT NO, STATION, OFFSET



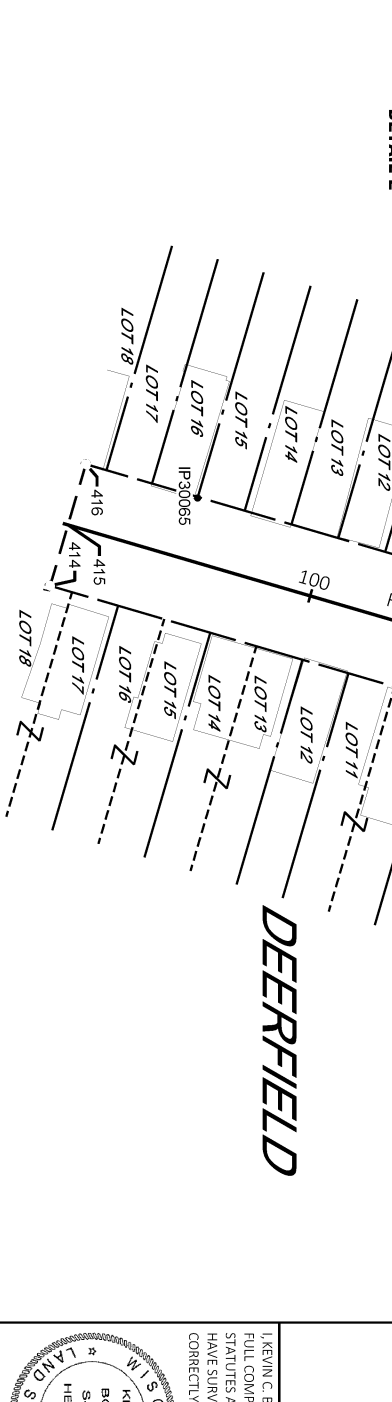
COURSE TABLE with columns: POINT NO., STATION, OFFSET, BEARING, DISTANCE



EXISTING MONUMENTS table with columns: POINT, Y (NORTHING), X (EASTING), DESCRIPTION



emcs.se logo and contact information for Kevin C. Boyer, Professional Land Surveyor.



Professional seal and signature of Kevin C. Boyer, Land Surveyor.

TRANSPORTATION PROJECT PLAT NO: 3070-04-20-4.05

BEING PART OF OUTLOT 19 OF THE ASSESSOR'S AND CONSOLIDATED PLAT OF THE VILLAGE OF DEERFIELD LOCATED IN PART OF THE NE1/4 OF THE SE1/4 OF SECTION 21, TOWNSHIP 7 NORTH, RANGE 12 EAST, VILLAGE OF DEERFIELD, DANE COUNTY, WISCONSIN

RELOCATION ORDER - STH 73, 139 TO COLUMBUS, SHAUL LANE TO NORTH STREET, DANE COUNTY

TO PROPERLY ESTABLISH LAY OUT, WIDTH, ENLARGE, EXTEND, CONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE PROJECT.

TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SECTION 84.02 (3), 84.09 AND 84.30, WISCONSIN STATUTES, THE DEPARTMENT OF TRANSPORTATION HEREBY ORDERS THE SAID HIGHWAY AS SHOWN ON THIS PLAT, LAY OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE PROJECT.

2. THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE DEPARTMENT FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE STATE OF WISCONSIN, PURSUANT TO THE PROVISIONS OF SECTION 84.09 (1) OR (2), WISCONSIN STATUTES.

SCHEDULE OF LANDS & INTERESTS REQUIRED

OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE DEPARTMENT.

Table with columns: PARCEL NUMBER, OWNERS, INTEREST, R/W SQ. FT. REQUIRED, TILE, SO. FT. TOTAL. Includes Adam P Bennett & Nicole L Biddick.

Table with columns: POINT, Y (NORTHING), X (EASTING), DESCRIPTION. Lists various points and their coordinates and descriptions like '1.75" O.D. IRON PIPE'.

NOTES:

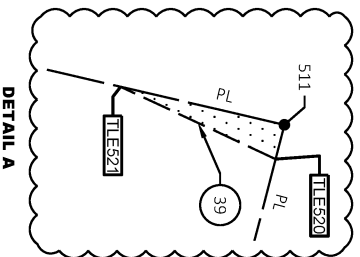
POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), DANE COUNTY, MADR3 (2011) IN US SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

FOR THE LATEST ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN MADISON, WI.

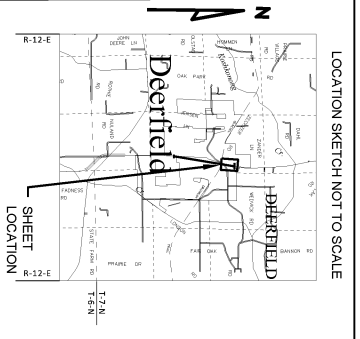
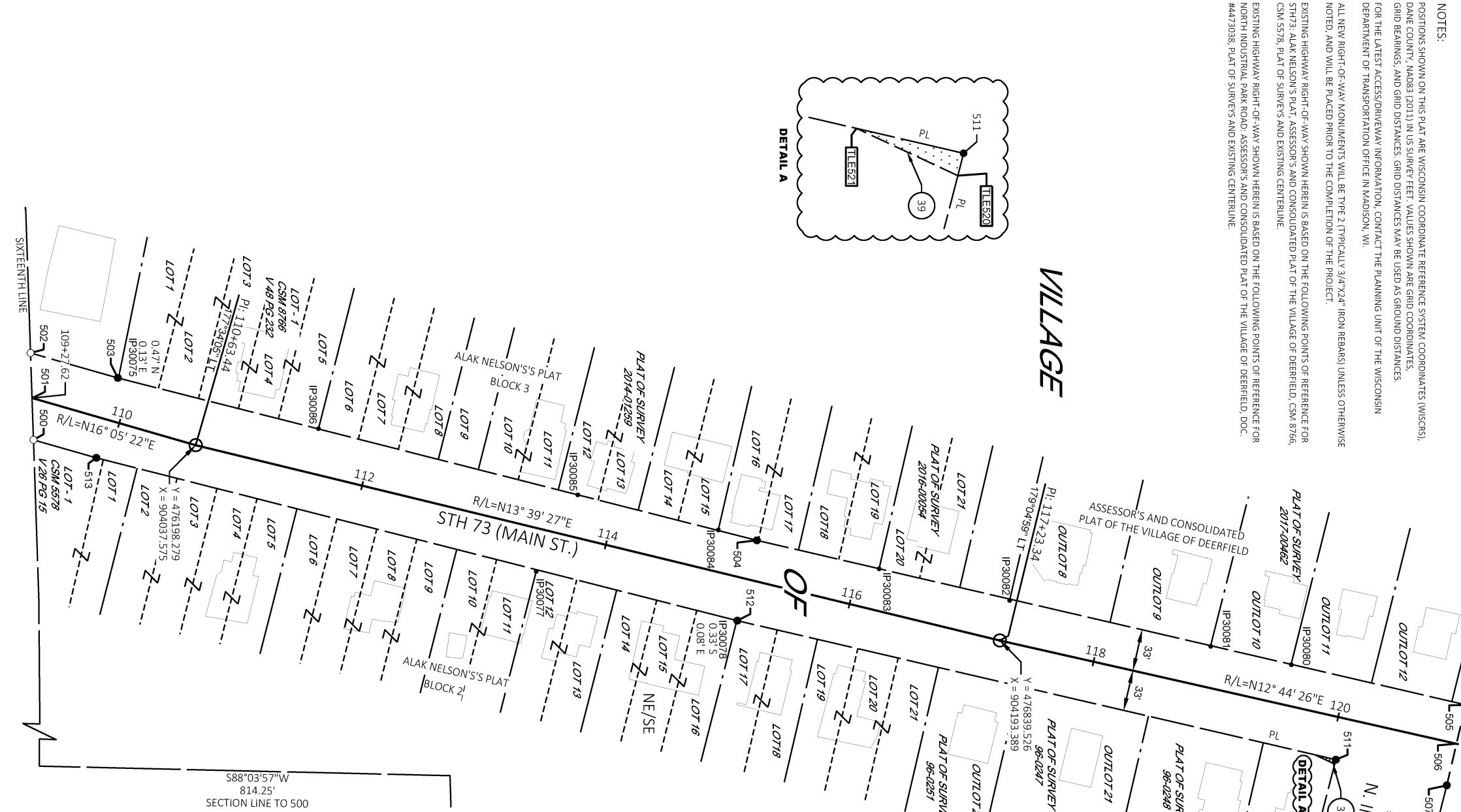
ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 3/4" X 24" IRON REBAR) UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE FOR STH73: ALAK NELSON'S PLAT, ASSESSOR'S AND CONSOLIDATED PLAT OF THE VILLAGE OF DEERFIELD, CSM 8766, CSM 5578, PLAT OF SURVEY'S AND EXISTING CENTERLINE.

EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE FOR NORTH INDUSTRIAL PARK ROAD, ASSESSOR'S AND CONSOLIDATED PLAT OF THE VILLAGE OF DEERFIELD, DOC. #4473038, PLAT OF SURVEY'S AND EXISTING CENTERLINE.



VILLAGE

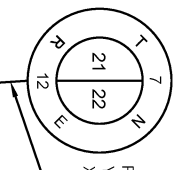


KRISTI CHLEBOSKI, DANE COUNTY REGISTER OF DEEDS. DOCUMENT # 5888500, 01/27/2021 11:44 AM. Includes registration information and a note about recording information.

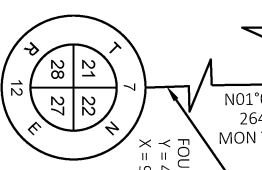
TILE STATION & OFFSET TABLE. Columns: POINT NO., STATION, OFFSET. Rows for TLES20 and TLES21.

COURSE TABLE. Columns: COURSE, BEARING, DISTANCE. Lists various courses with bearings and distances.

STATION & OFFSET TABLE. Columns: POINT NO., STATION, OFFSET. Lists stationing points and their offsets.

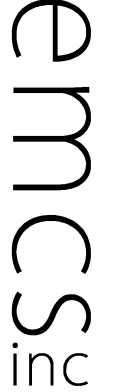


DEERFIELD



FOUND 75" IRON ROD X = 477416.238 Y = 904870.260. FOUND BERTINSEN MONUMENT X = 904823.882 Y = 474776.536.

ALL TILES ARE FOR SLOPE GRADING PURPOSES. REFER TO THE TITLE SHEET, RECORDED AS SHEET 2 OF 2, IN TRANSPORTATION PROJECT PLATS AS DOCUMENT NO. 5688205 FOR ADDITIONAL INFORMATION.



Professional seal and signature of Kevin C. Boyer, Wisconsin Land Surveyor. Includes registration number 5-2675 and date 01/26/2021.

TRANSPORTATION PROJECT PLAT NO: 3070-04-20-4.06

BEING PART OF OUTLOT 14 AND OUTLOT 15 OF THE ASSESSOR'S AND CONSOLIDATED PLAT OF THE VILLAGE OF DEERFIELD LOCATED IN PART OF THE NE/4 OF THE SE/4 AND PART OF LOT 1, BLOCK 1 AND PART OF LOT 1 BLOCK 2 OF HOWE'S ADDITION TO THE VILLAGE OF DEERFIELD LOCATED IN PART OF THE SE/4 OF THE NE/1/4, ALL LOCATED IN SECTION 21, TOWNSHIP 7 NORTH, RANGE 12 EAST, VILLAGE OF DEERFIELD, DANE COUNTY, WISCONSIN

RELOCATION ORDER - STH 73, 139 TO COLUMBUS, SHAUL LANE TO NORTH STREET, DANE COUNTY TO PROPERTY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DENIES IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE PROJECT

Table with 5 columns: PARCEL NUMBER, OWNERS, INTEREST, R/W SQ. FT., REQUIRED NEW EXISTING TOTAL SQ. FT. TILES

Table with 2 columns: POINT, DESCRIPTION

NOTES: POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM... DANE COUNTY, MADISON (2011) IN US SURVEY FEET...

EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE FOR STH73: ASSESSOR'S AND CONSOLIDATED PLAT OF THE VILLAGE OF DEERFIELD, HOWE'S ADDITION TO THE VILLAGE OF DEERFIELD, AND EXISTING CENTERLINE.

EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE FOR TRUMAN DRIVE: ASSESSOR'S AND CONSOLIDATED PLAT OF THE VILLAGE OF DEERFIELD, HOWE'S ADDITION TO THE VILLAGE OF DEERFIELD, AND EXISTING CENTERLINE.

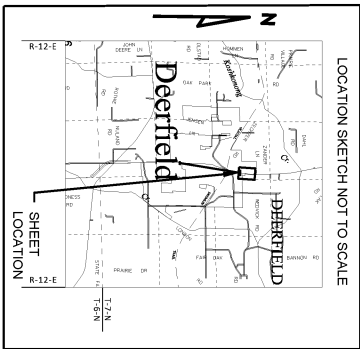
EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE FOR NORTH STREET: HOWE'S ADDITION TO THE VILLAGE OF DEERFIELD, AND EXISTING CENTERLINE.

Table: TILE STATION & OFFSET TABLE with columns: POINT NO., STATION, OFFSET

Table: STATION & OFFSET TABLE with columns: POINT NO., STATION, OFFSET

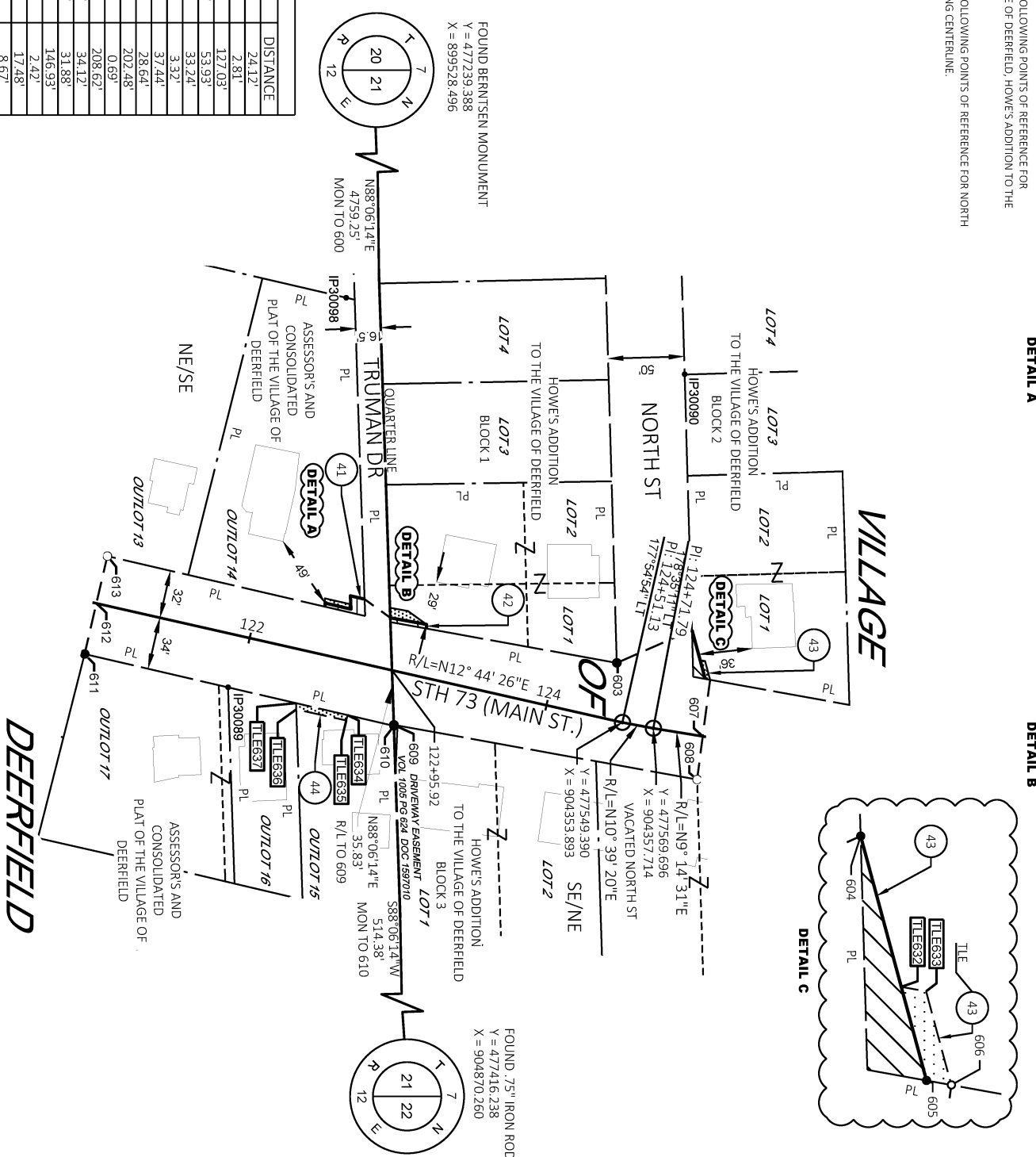
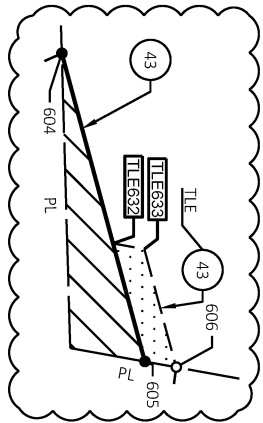
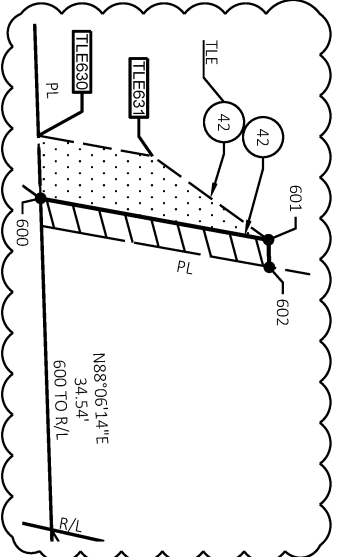
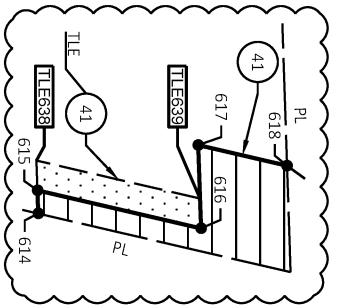
Table: COURSE TABLE with columns: COURSE, BEARING, DISTANCE

SCALE, FEET 0 50 100



KRSTI CIELEBOWSKI DANE COUNTY REGISTER OF DEEDS DOCUMENT # 5688362 01/27/2021 11:44 AM

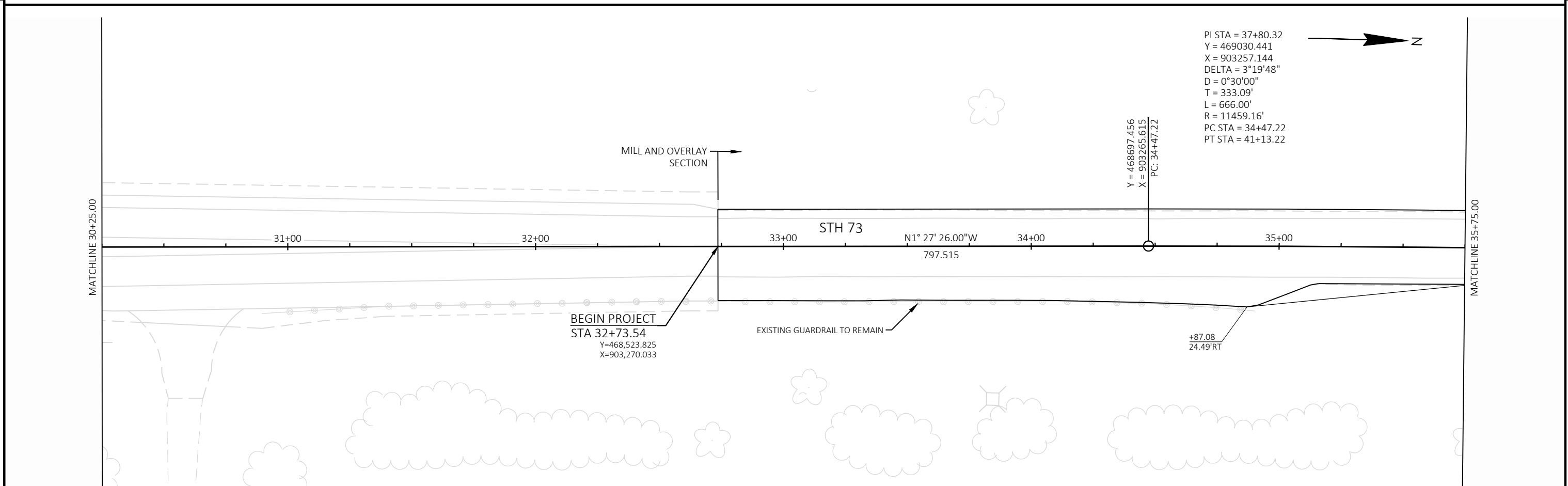
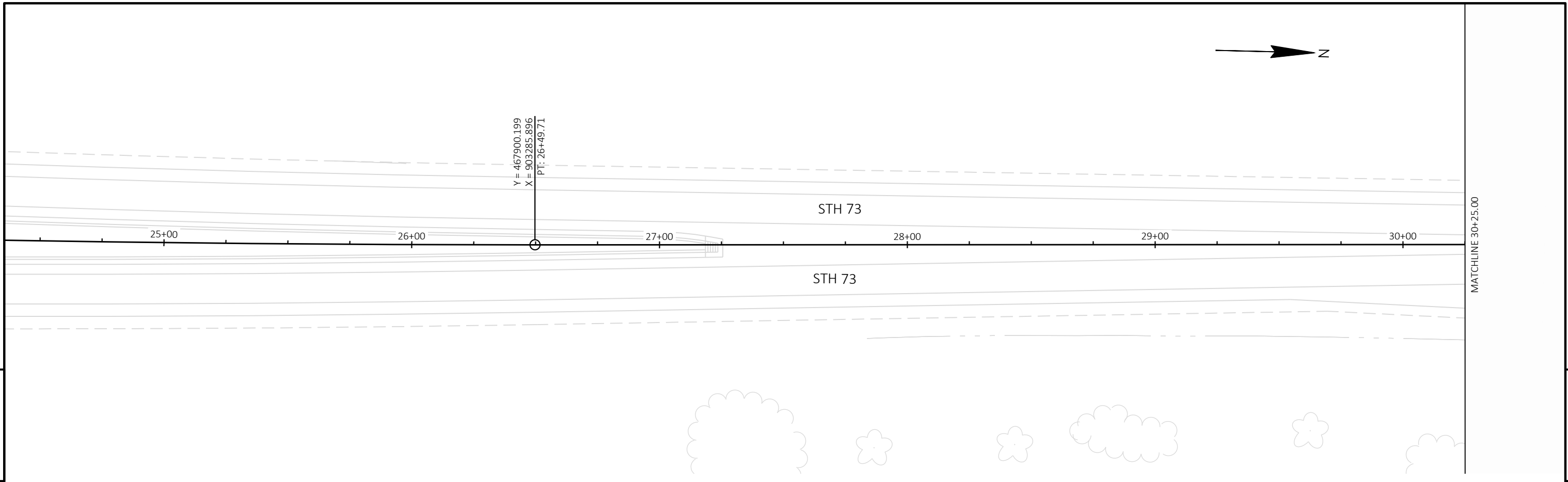
RESERVED FOR REGISTER OF DEEDS PROJECT NUMBER 3070-04-20-4.06 AMENDMENT NO. _____



emcs LLC logo and contact information for Kevin C. Bover, Wisconsin Land Surveyor, including address and phone number.

REFER TO THE TITLE SHEET, RECORDED AS SHEET 2 OF 2, IN TRANSPORTATION PROJECT PLATS AS DOCUMENT NO. 5686205 FOR ADDITIONAL INFORMATION.

ALL TILES ARE FOR SLOPE GRADING PURPOSES

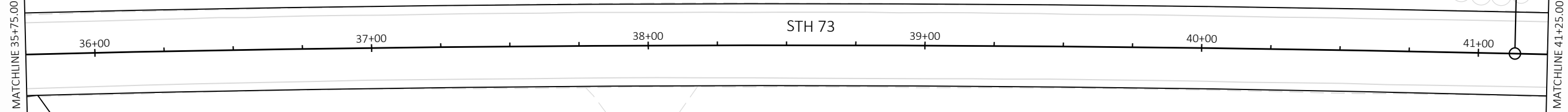


PROJECT NO: 3070-04-60	HWY: STH 73	COUNTY: DANE	PLAN DETAILS	SHEET	E
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PI STA = 37+80.32
 Y = 469030.441
 X = 903257.144
 DELTA = 3°19'48"
 D = 0°30'00"
 T = 333.09'
 L = 666.00'
 R = 11459.16'
 PC STA = 34+47.22
 PT STA = 41+13.22



Y = 469363.355
 X = 903268.029
 PT: 41+13.22

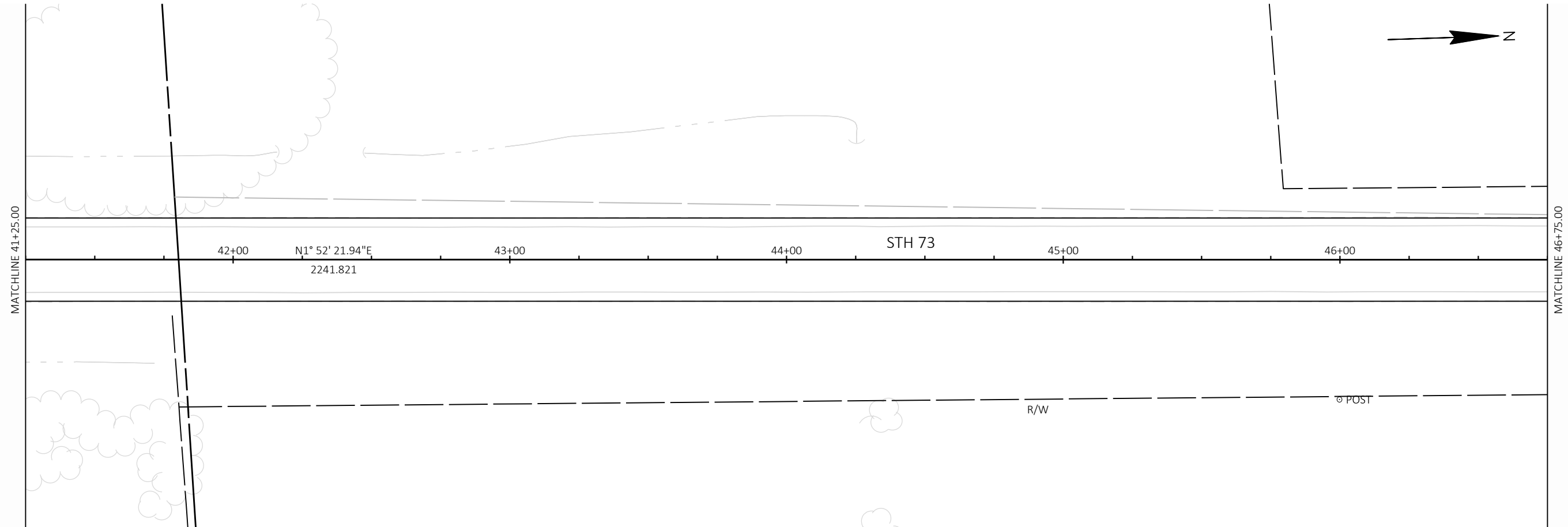


+78.99
14.83' RT

SIGN

5

5



N1° 52' 21.94"E
 2241.821



R/W

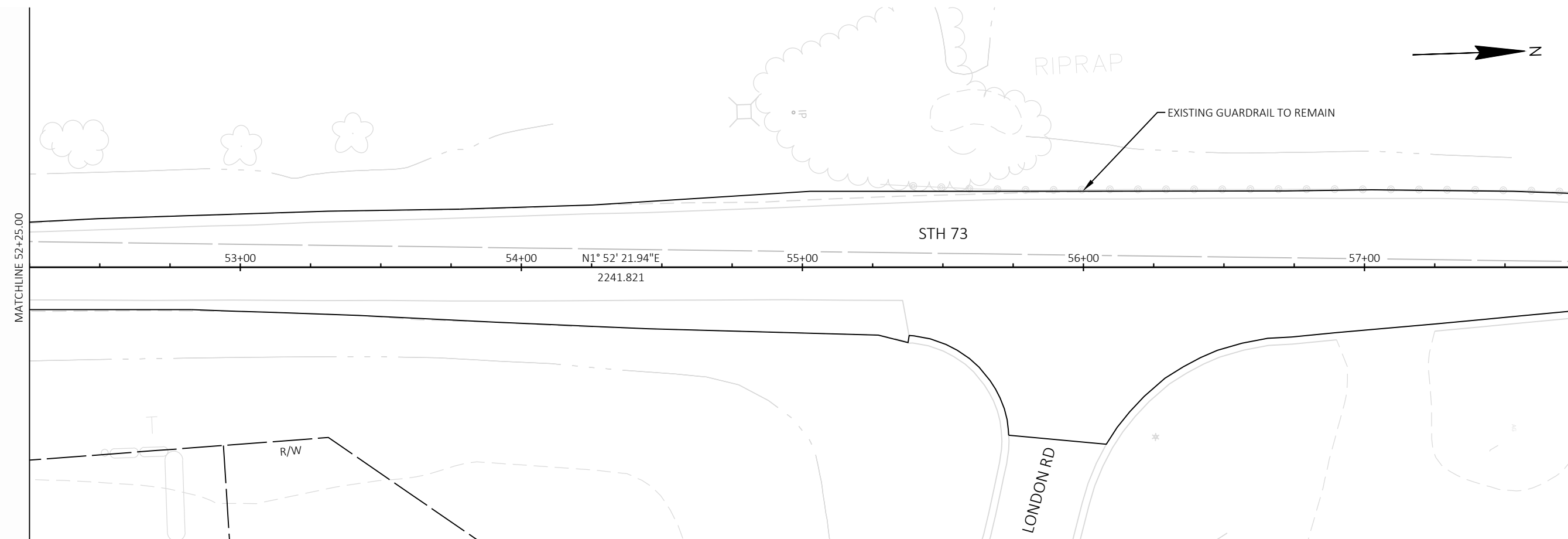
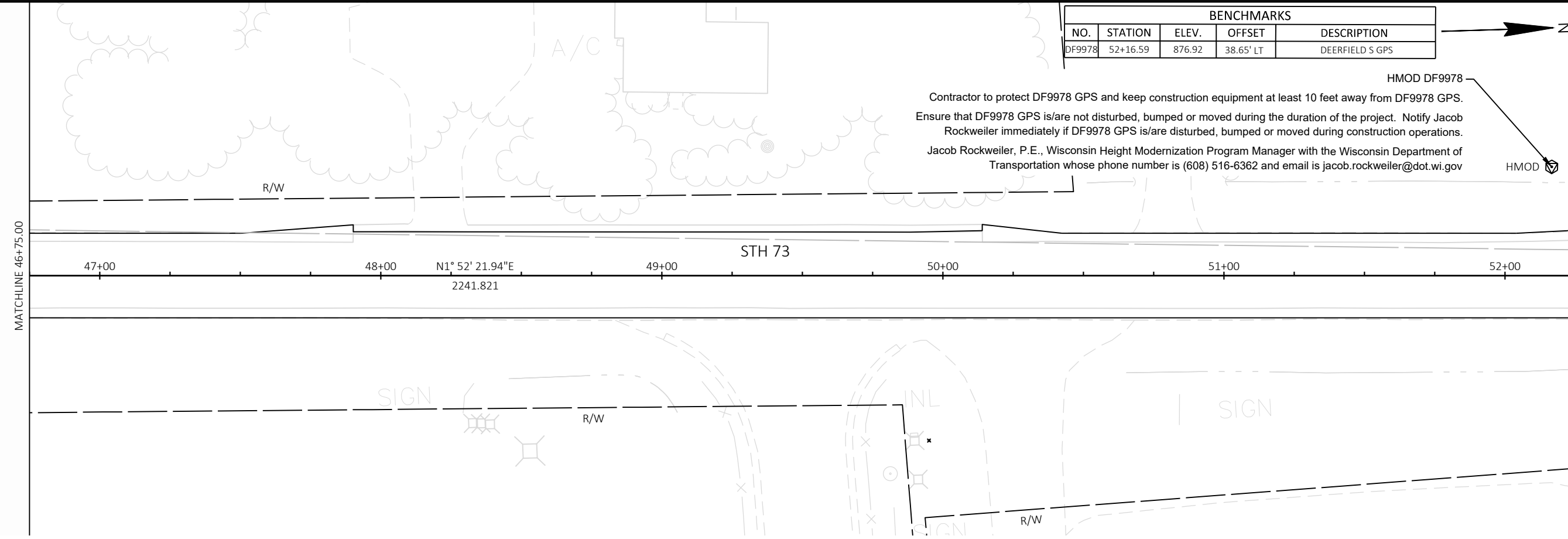
POST

BENCHMARKS				
NO.	STATION	ELEV.	OFFSET	DESCRIPTION
DF9978	52+16.59	876.92	38.65' LT	DEERFIELD S GPS

Contractor to protect DF9978 GPS and keep construction equipment at least 10 feet away from DF9978 GPS. Ensure that DF9978 GPS is/are not disturbed, bumped or moved during the duration of the project. Notify Jacob Rockweiler immediately if DF9978 GPS is/are disturbed, bumped or moved during construction operations. Jacob Rockweiler, P.E., Wisconsin Height Modernization Program Manager with the Wisconsin Department of Transportation whose phone number is (608) 516-6362 and email is jacob.rockweiler@dot.wi.gov

HMOD DF9978

HMOD



5

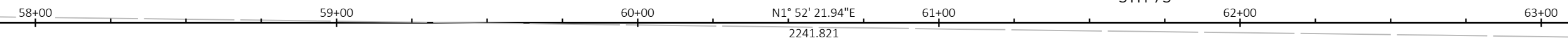
5



PROFILE CURB CUT
STA 61+06 - STA 62+06

EXISTING GUARDRAIL TO REMAIN

STH 73



MATCHLINE 57+75.00

MATCHLINE 63+25.00

5

5

Y = 471603.979
X = 903341.292
PI: 63+55.04

GOODLAND AVE

STH 73

FRONTIER - FIBER OPTIC

FRONTIER - TELECOMMUNICATIONS

CHARTER - FIBER OPTIC

ALLIANT ENERGY - ELECTRIC

ALLIANT ENERGY - GAS

DEERFIELD WATER UTILITY - WATER

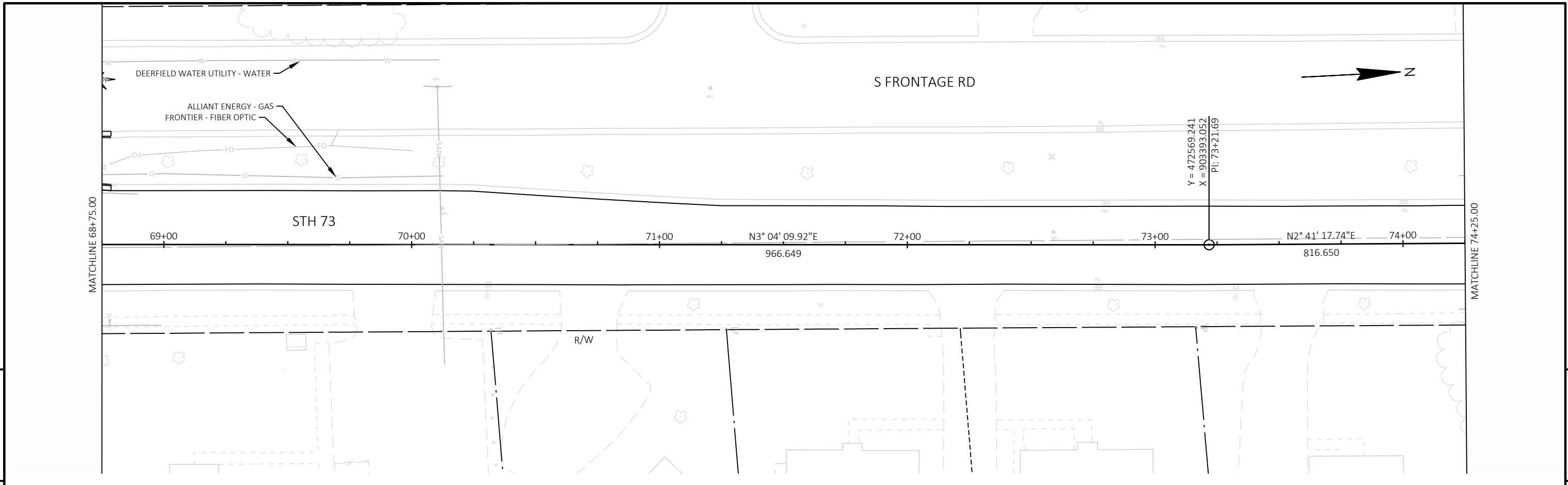
DEERFIELD WATER UTILITY - SANITARY

DEERFIELD WATER UTILITY - STORM SEWER

BENCHMARKS				
NO.	STATION	ELEV.	OFFSET	DESCRIPTION
CP529	68+37.15	880.800	36.54' RT	CP 0.75 IN IR W/CAP

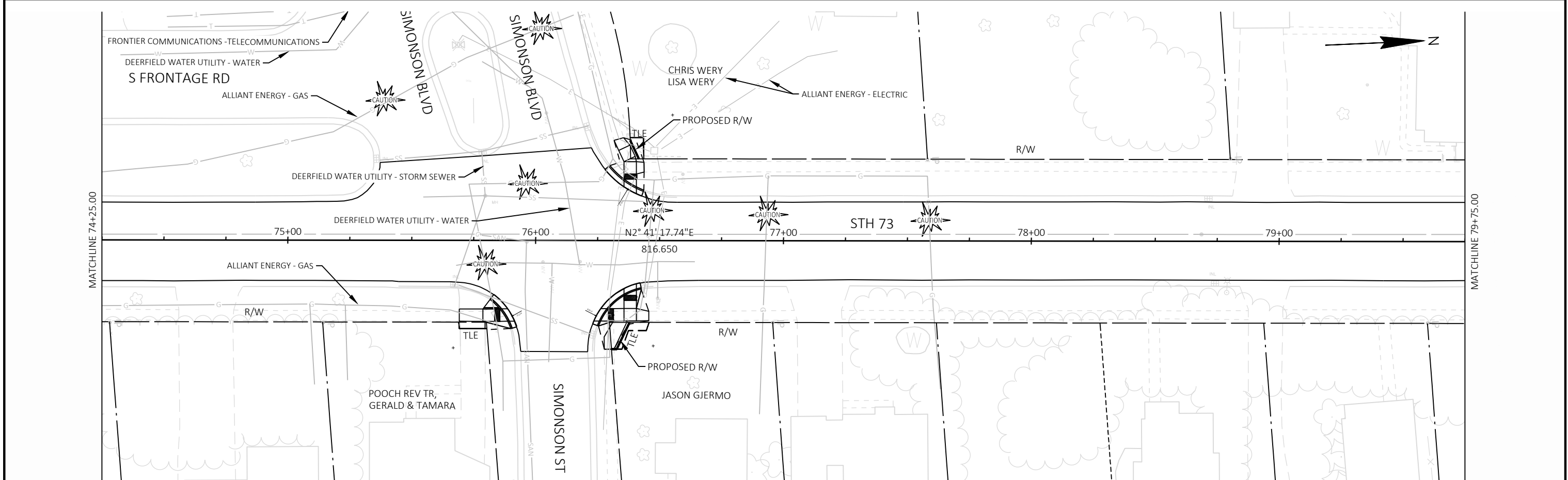
MATCHLINE 63+25.00

MATCHLINE 68+75.00

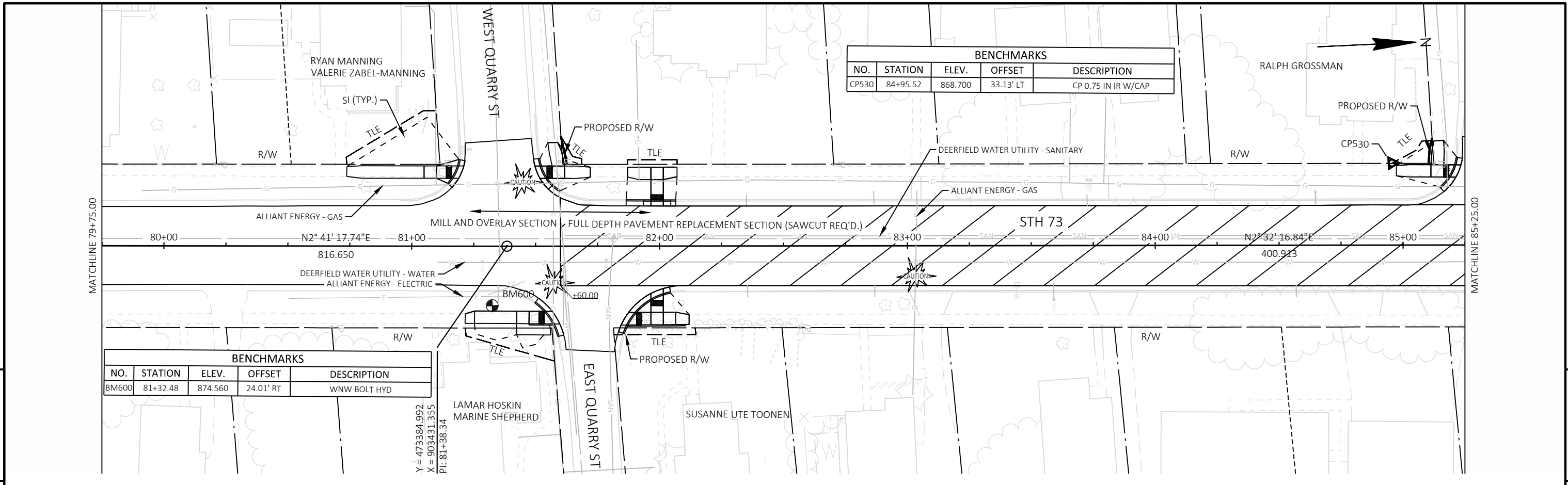


5

5



PROJECT NO: 3070-04-60	HWY: STH 73	COUNTY: DANE	PLAN DETAILS	SHEET	E
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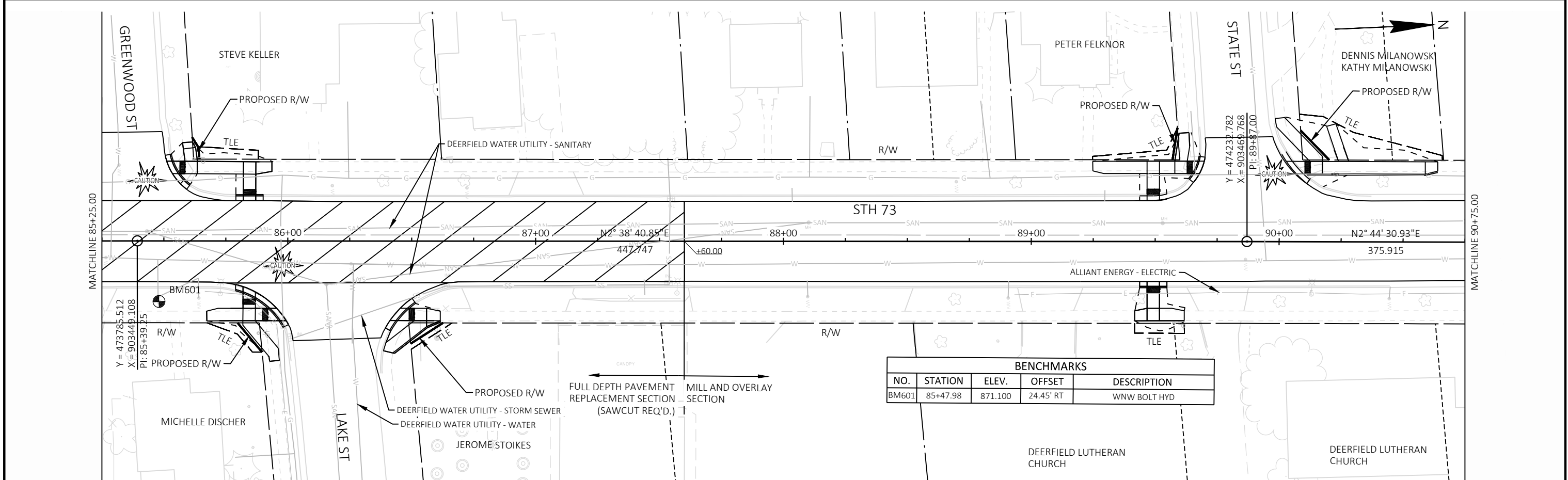
BENCHMARKS				
NO.	STATION	ELEV.	OFFSET	DESCRIPTION
CP530	84+95.52	868.700	33.13' LT	CP 0.75 IN IR W/CAP

BENCHMARKS				
NO.	STATION	ELEV.	OFFSET	DESCRIPTION
BM600	81+32.48	874.560	24.01' RT	WNW BOLT HYD

Y = 473384.992
X = 903431.355
PI: 81+38.34

5

5



BENCHMARKS				
NO.	STATION	ELEV.	OFFSET	DESCRIPTION
BM601	85+47.98	871.100	24.45' RT	WNW BOLT HYD

Y = 473785.512
X = 903449.108
PI: 85+39.25

BENCHMARKS				
NO.	STATION	ELEV.	OFFSET	DESCRIPTION
BM602	91+17.07	878.340	23.44' RT	WNW BOLT HYD

BENCHMARKS				
NO.	STATION	ELEV.	OFFSET	DESCRIPTION
CP531	93+12.10	875.580	33.16' LT	CP 0.75 IN IR W/CAP

BENCHMARKS				
NO.	STATION	ELEV.	OFFSET	DESCRIPTION
BM603	95+52.50	874.270	25.36' RT	WNW BOLT HYD

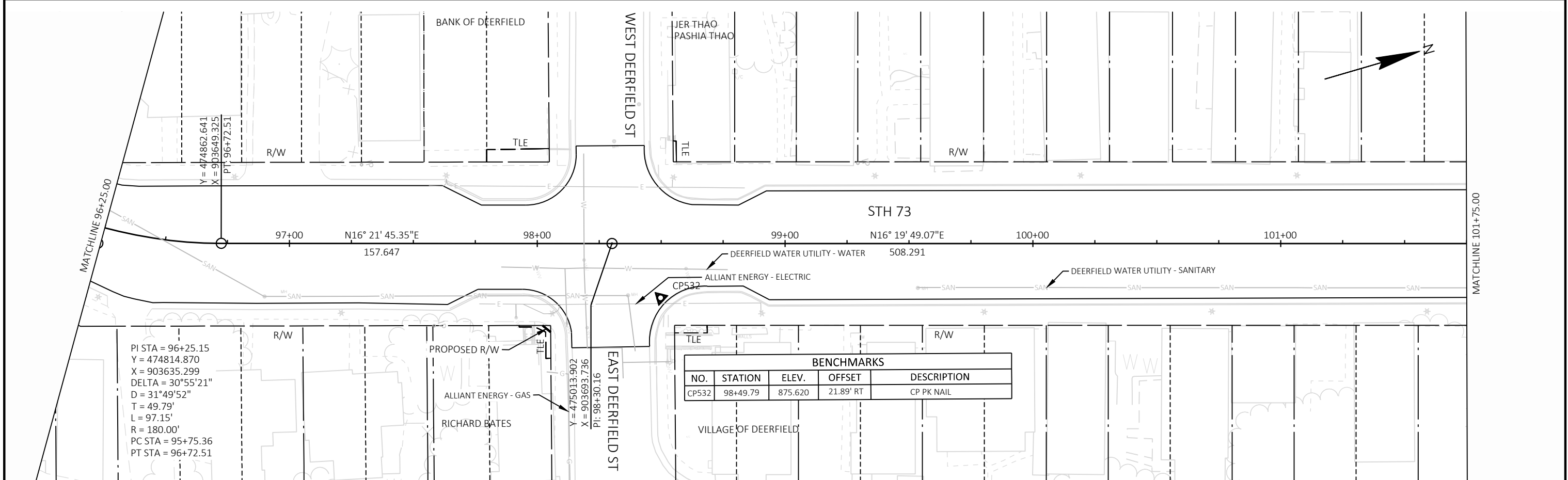
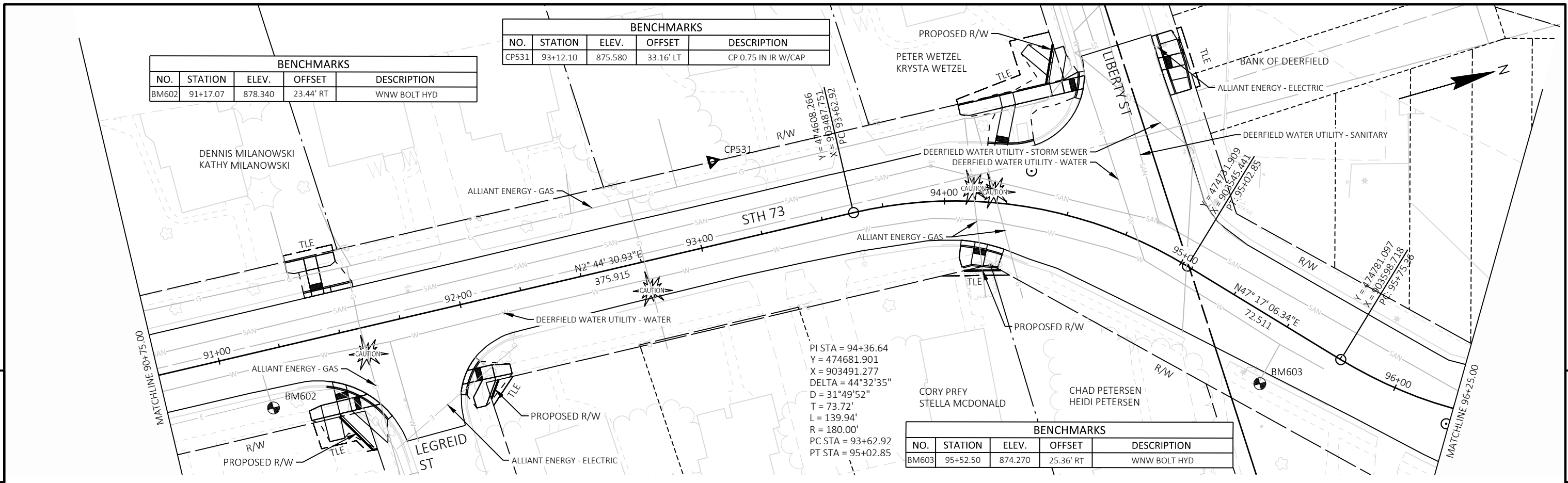
BENCHMARKS				
NO.	STATION	ELEV.	OFFSET	DESCRIPTION
CP532	98+49.79	875.620	21.89' RT	CP PK NAIL

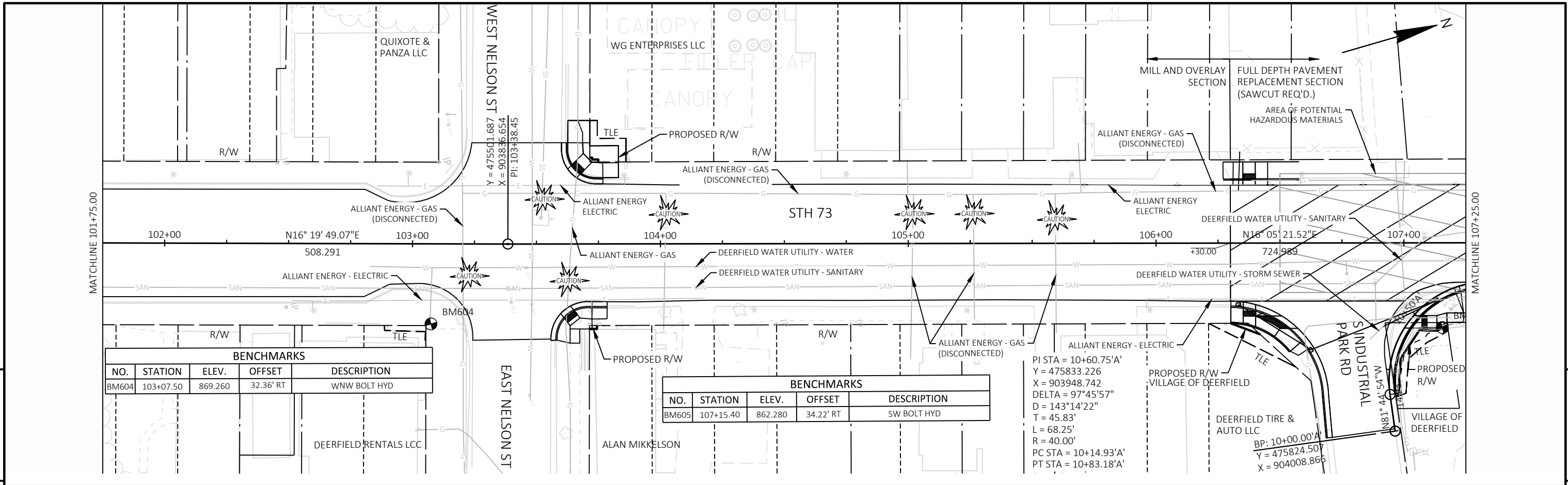
PI STA = 96+25.15
 Y = 474814.870
 X = 903635.299
 DELTA = 30°55'21"
 D = 31°49'52"
 T = 49.79'
 L = 97.15'
 R = 180.00'
 PC STA = 95+75.36
 PT STA = 96+72.51

Y = 475013.902
 X = 903693.736
 PI: 98+30.16

Y = 474862.641
 X = 903649.325
 PI: 96+72.51

PI STA = 94+36.64
 Y = 474681.901
 X = 903491.277
 DELTA = 44°32'35"
 D = 31°49'52"
 T = 73.72'
 L = 139.94'
 R = 180.00'
 PC STA = 93+62.92
 PT STA = 95+02.85





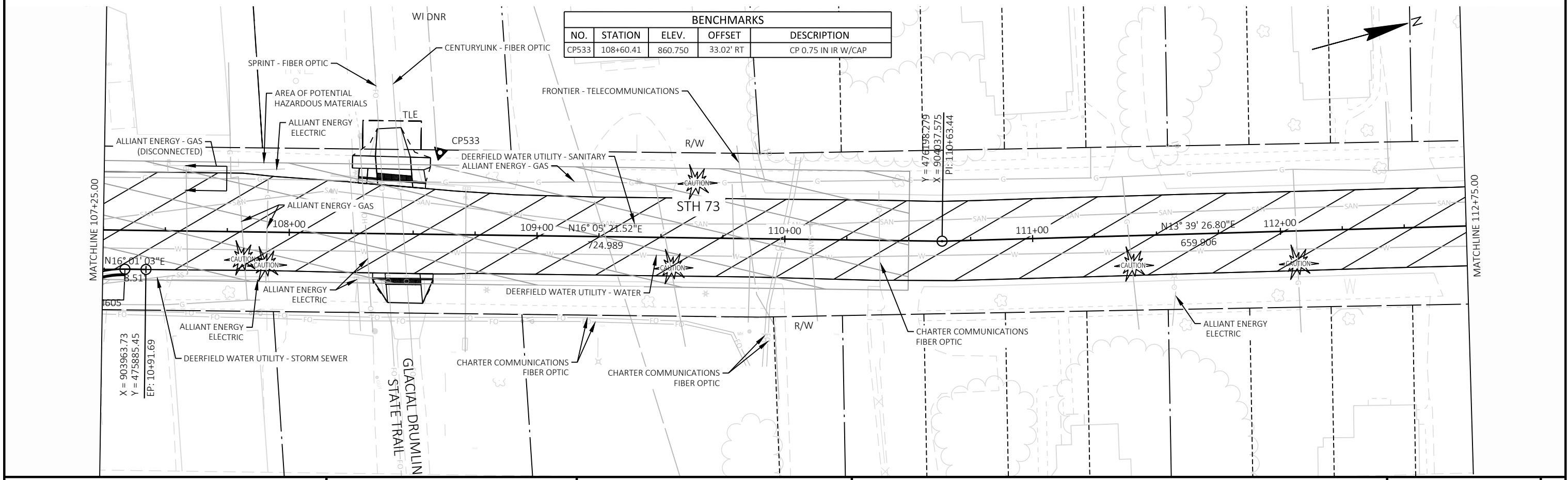
BENCHMARKS				
NO.	STATION	ELEV.	OFFSET	DESCRIPTION
BM604	103+07.50	869.260	32.36' RT	WNW BOLT HYD

BENCHMARKS				
NO.	STATION	ELEV.	OFFSET	DESCRIPTION
BM605	107+15.40	862.280	34.22' RT	SW BOLT HYD

PI STA = 10+60.75'A'
 Y = 475833.226
 X = 903948.742
 DELTA = 97°45'57"
 D = 143°14'22"
 T = 45.83'
 L = 68.25'
 R = 40.00'
 PC STA = 10+14.93'A'
 PT STA = 10+83.18'A'

5

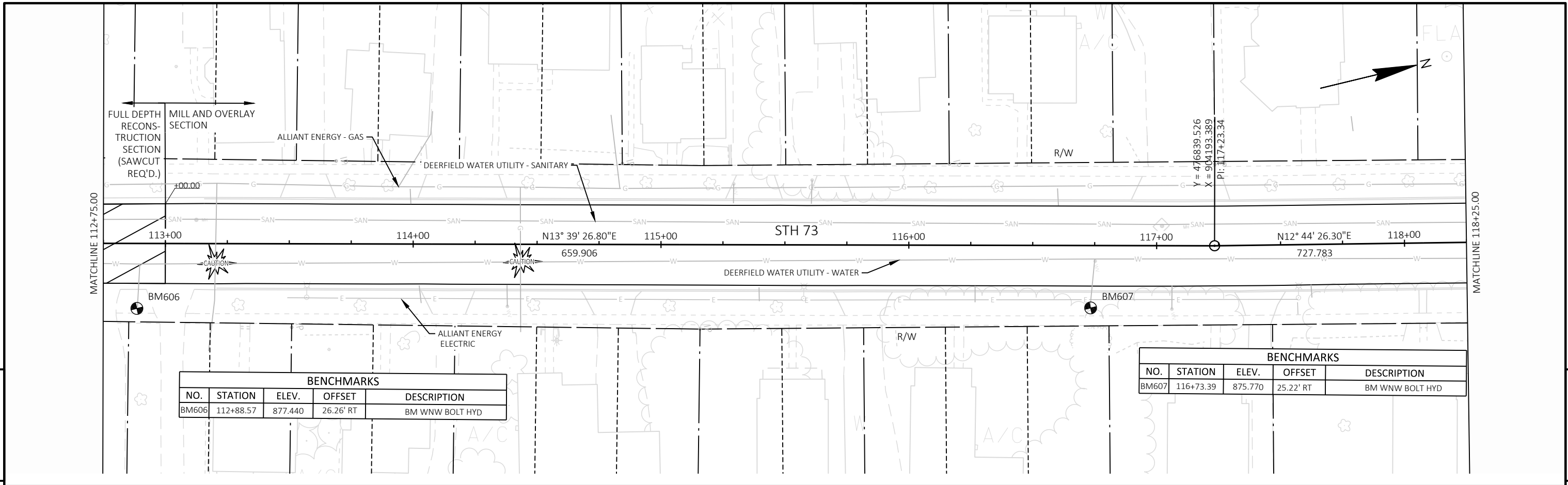
5



BENCHMARKS				
NO.	STATION	ELEV.	OFFSET	DESCRIPTION
CP533	108+60.41	860.750	33.02' RT	CP 0.75 IN IR W/CAP

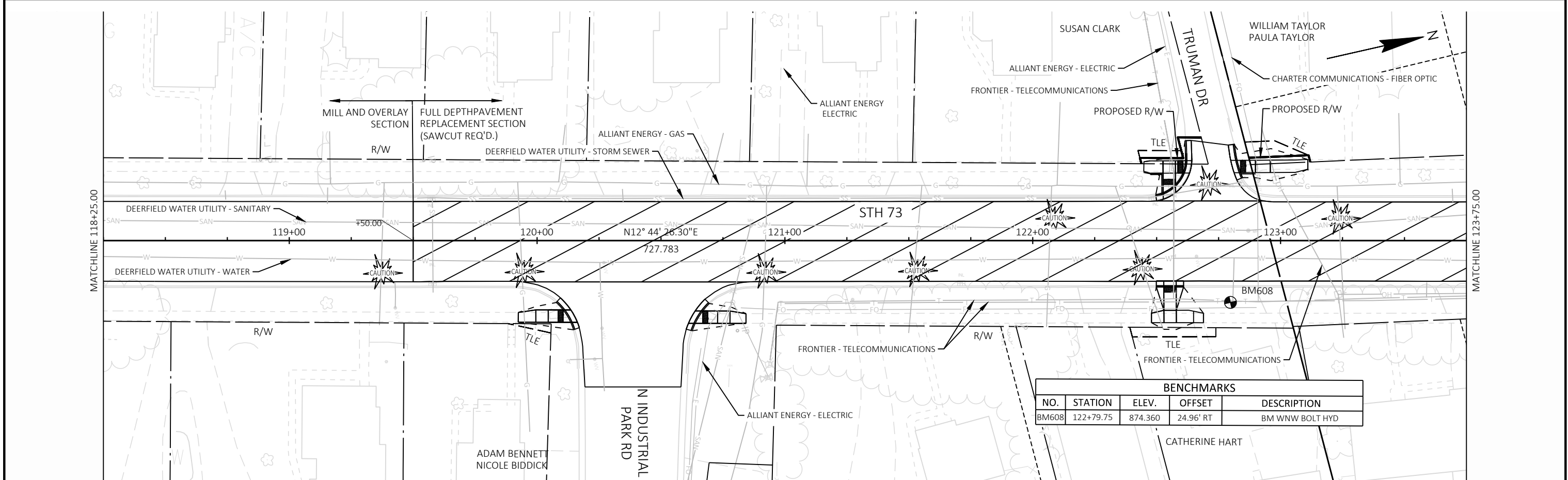
X = 903963.73
 Y = 475885.45
 EP: 10+91.69

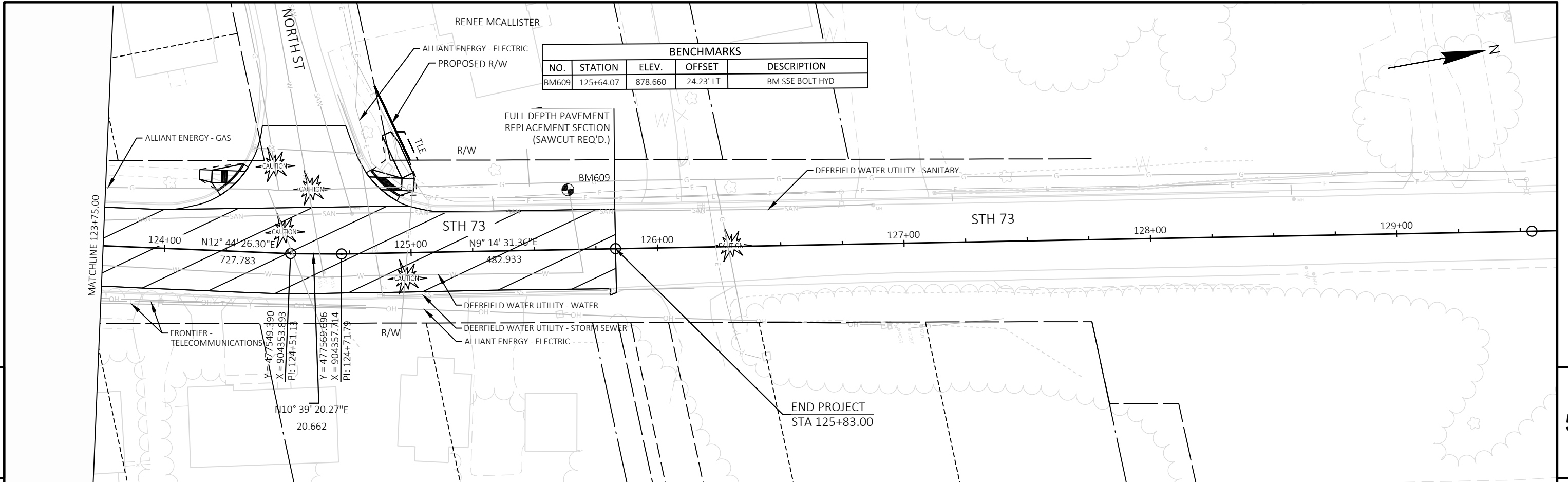
Y = 476198.279
 X = 904037.575
 PI: 110+63.44



5

5





PROJECT NO: 3070-04-60

HWY: STH 73

COUNTY: DANE

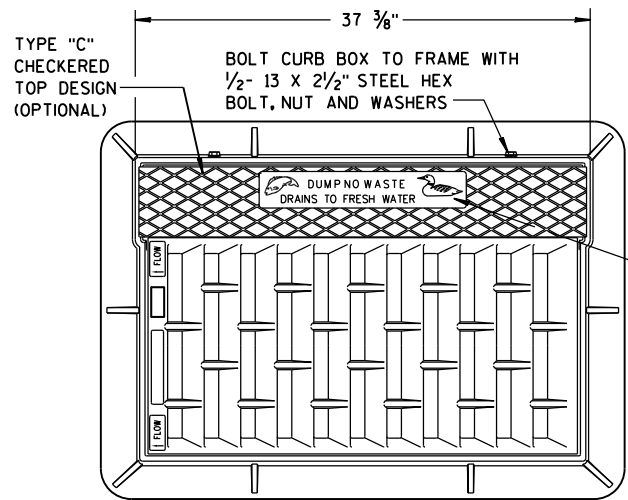
PLAN DETAILS

SHEET

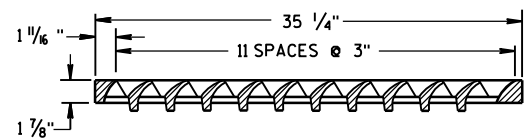
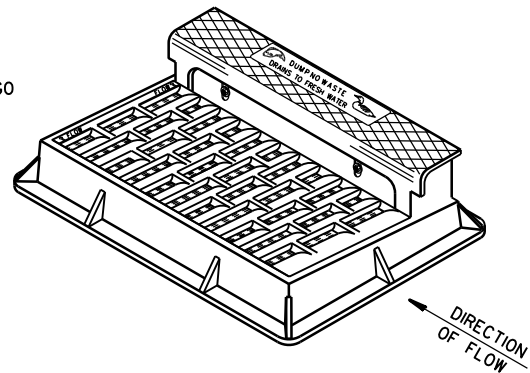
E

Standard Detail Drawing List

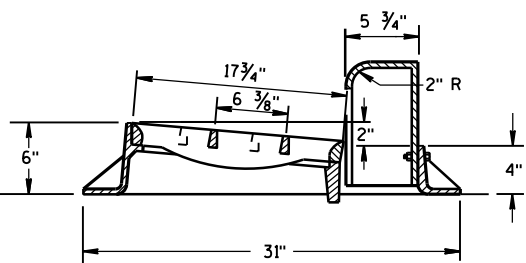
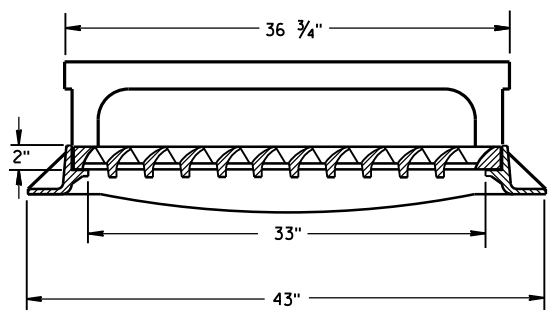
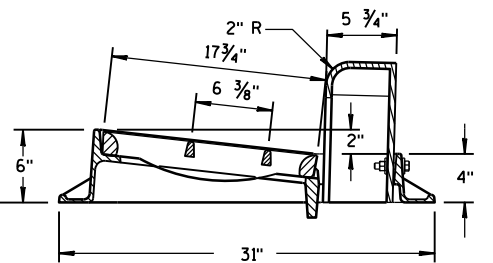
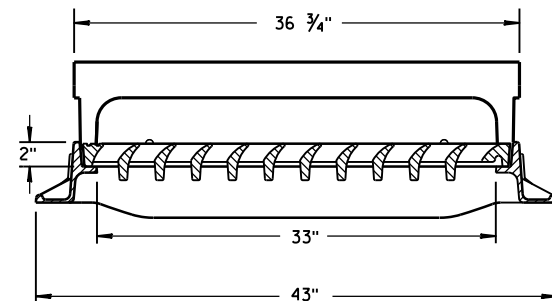
08A05-19A	INLET COVERS TYPE A, H, A-S, H-S & Z
08C06-02	INLETS 3-FT AND 4-FT DIAMETER
08D01-22A	CONCRETE CURB & GUTTER
08D01-22B	CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS
08D05-20A	CURB RAMPS TYPES 1 AND 1-A
08D05-20B	CURB RAMPS TYPES 2 AND 3
08D05-20C	CURB RAMPS TYPES 4A AND 4A1
08D05-20D	CURB RAMPS TYPE 4B AND 4B1
08D05-20E	CURB RAMPS TYPES 5, 6, 7A, 7B & 8
08D05-20F	CURB RAMPS RADIAL DETECTABLE WARNING FIELD APPLICATIONS
08D05-20G	CURB RAMPS RECTANGULAR AND RADIAL DETECTABLE WARNING PLATES
08D19-03	DRIVEWAY AND SIDEWALK RAMPS TYPE Z
08E10-02	INLET PROTECTION TYPE A, B, C AND D
11B02-02	CONCRETE MEDIAN NOSE
13A11-03A	2-LANE RURAL CENTER LINE RUMBLE STRIP, MILLING
13A11-03B	2-LANE RURAL CENTER LINE RUMBLE STRIP, MILLING
13C19-03	HMA LONGITUDINAL JOINTS
15C02-08A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C03-05	BARRICADES AND SIGNS FOR SIDEROAD CLOSURES
15C04-05	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M. P. H. OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC
15C05-05	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M. P. H. OR LESS
15C07-15C	PAVEMENT MARKING ARROWS
15C07-15E	PAVEMENT MARKING FOR BIKE LANES
15C08-21A	LONGITUDINAL MARKING (MAINLINE)
15C08-21B	TEMPORARY LONGITUDINAL PAVEMENT MARKING
15C08-21C	PAVEMENT MARKING (TURN LANES)
15C08-21D	PAVEMENT MARKING (TURN LANES)
15C12-09A	TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION
15C18-06B	MEDIAN ISLAND MARKING MEDIAN ISLAND NOSE
15C19-07A	MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY
15C33-04	STOP LINE AND CROSSWALK PAVEMENT MARKING
15C35-05A	PAVEMENT MARKING (INTERSECTIONS)
15D30-07B	TRAFFIC CONTROL, TEMPORARY ADA COMPLIANT PEDESTRIAN ACCOMMODATION
15D30-07F	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-07I	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-07J	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D39-02	TRAFFIC CONTROL, DROP-OFF SIGNING



**NOTE:
GRATE IS REVERSIBLE.**

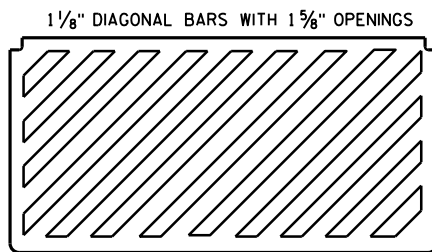


NOTE: CURB BOX HEIGHT ADJUSTABLE 6" TO 9"

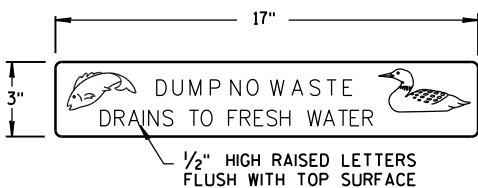


TYPE "H"

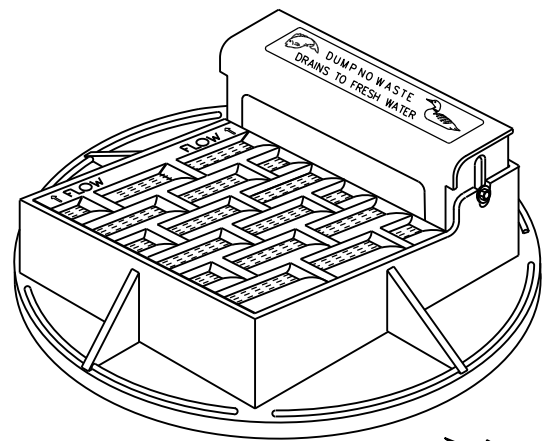
NOTE: EITHER CASTING IS ACCEPTABLE



**SPECIAL GRATE FOR
TYPE "H" COVER**
(MEASURES 35 1/4" X 17 3/4" X 2")
(NOTED AS TYPE H-S ON DRAINAGE TABLE)

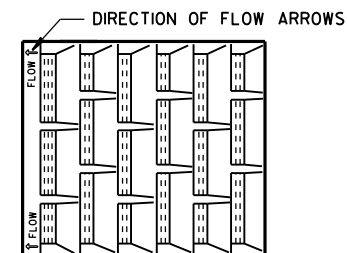


LOGO DETAIL

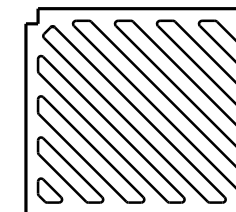


NOTE: CURB BOX ADJUSTABLE 4" TO 9"

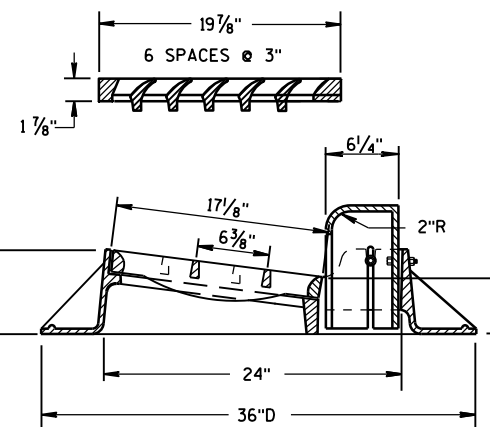
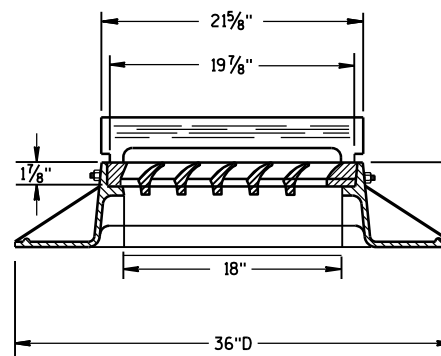
**NOTE:
GRATE IS REVERSIBLE.**



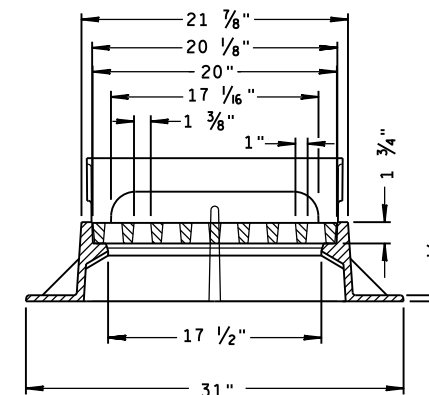
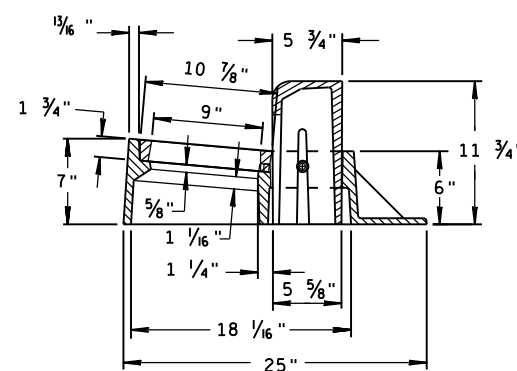
**1" DIAGONAL BARS
WITH 1 1/2" OPENINGS**



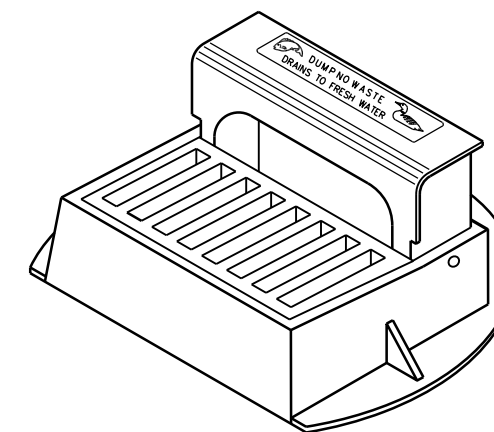
**SPECIAL GRATE FOR
TYPE "A" COVER**
(MEASURES 19 3/4" X 17" X 1 1/8")
(NOTED AS TYPE A-S ON DRAINAGE TABLE)



TYPE "A"



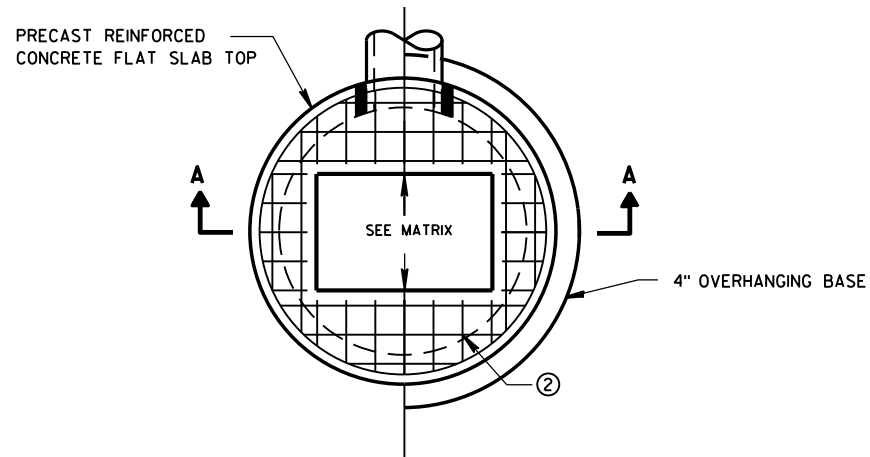
TYPE "Z"



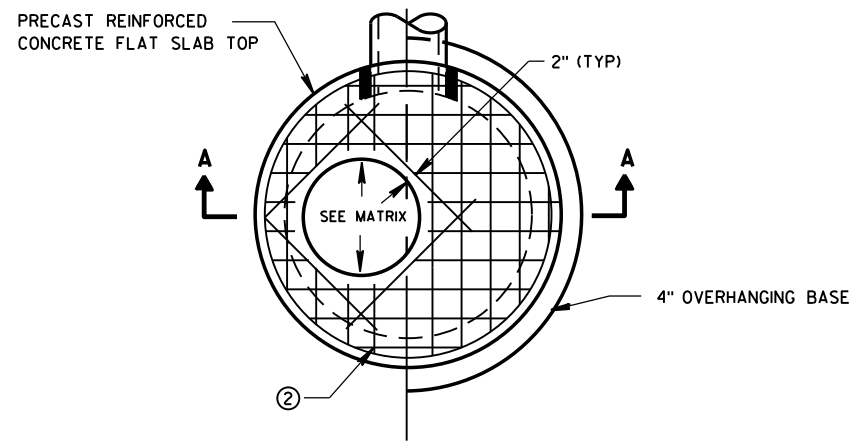
**INLET COVERS
TYPE A, H, A-S, H-S & Z**

**STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION**

APPROVED
11-27-13
DATE /S/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT
ENGINEER
FHWA



PLAN VIEW RECTANGULAR OPENING



PLAN VIEW CIRCULAR OPENING

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST INLET UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL DRAINAGE STRUCTURES ARE DESIGNATED ON THE PLANS AS "MANHOLES 3X3-L", "CATCH BASINS 4-B", "INLETS 2X3-H", ETC. THE FIRST NUMBERS DESIGNATE THE SIZE OF THE STRUCTURE, AND THE FOLLOWING LETTER DESIGNATES THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF FOUNDATION BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

ALL PRECAST INLET UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF AASHTO DESIGNATION M199.

PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

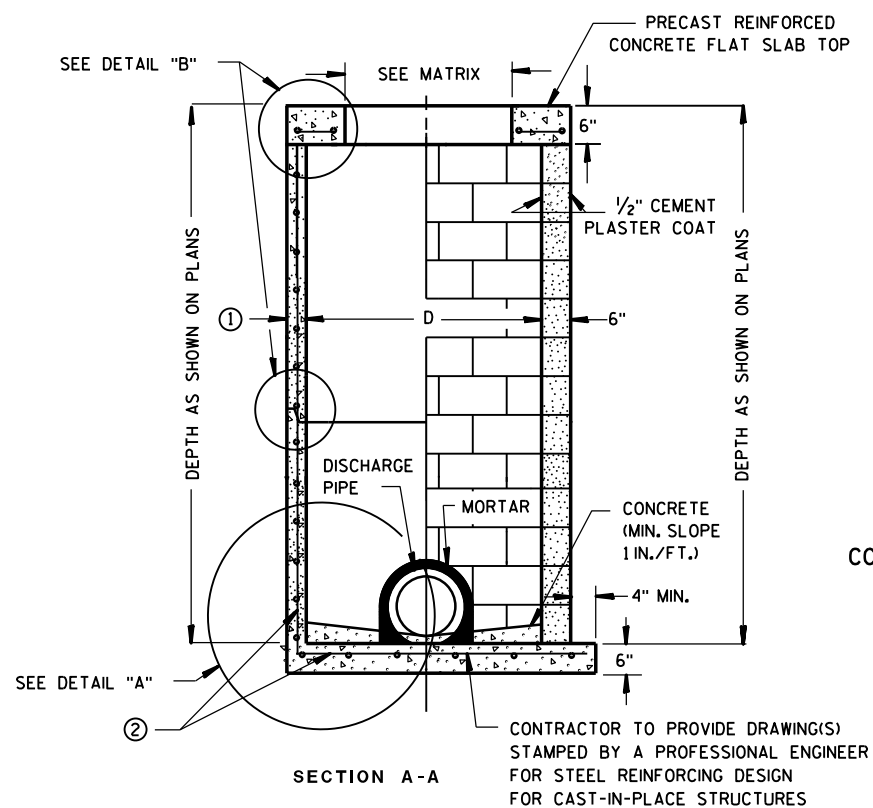
4" OVERHANGING BASES ARE REQUIRED FOR ALL CONCRETE BLOCK INSTALLATIONS. 4" OVERHANG IS REQUIRED WHEN SEPARATE PRECAST BASE IS PROVIDED. OVERHANG IS NOT REQUIRED ON PRECAST STRUCTURES WITH AN INTEGRAL OR MONOLITHIC BASE.

FOR ADDITIONAL CONFIGURATIONS, MAINTAIN A MINIMUM OF 12 INCHES AS MEASURED FROM THE INSIDE OF THE STRUCTURE WALL BETWEEN THE OUTSIDE PIPE WALLS OF ADJACENT PIPES. SEE DETAIL "C".

- ① MINIMUM WALL THICKNESS SHALL BE 4-IN FOR 3-FT DIAMETER AND 5-IN FOR 4-FT DIAMETER PRECAST INLETS.
- ② FOR PRECAST CATCH BASINS PROVIDE REINFORCING STEEL IN ACCORDANCE TO AASHTO M199.

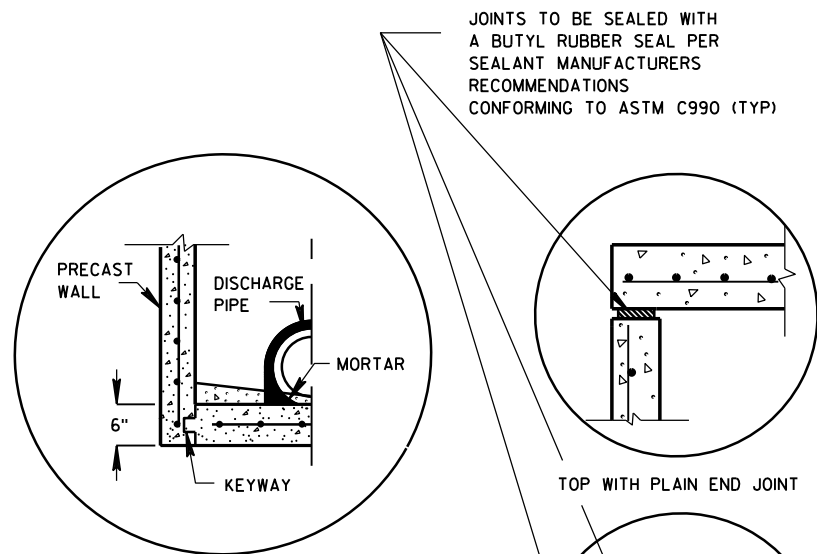
INLET COVER OPENING MATRIX

	INLET COVER TYPE	ALL A'S	ALL B'S	BW	C	F	ALL H'S	S	T	V	WM	Z
3-FT	2 DIA.				X							X
	2X2	X	X					X		X		
4-FT	2 DIA.				X							X
	2X2	X	X					X		X	X	
	2X2.5			X								
	2X3						X					
	2.5X3					X						

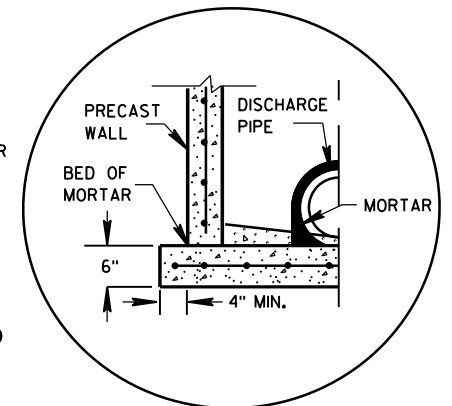


PRECAST REINFORCED CONCRETE WITH MONOLITHIC BASE OR CONCRETE BLOCK WITH CAST-IN-PLACE OR PRECAST REINFORCED CONCRETE BASE ②

CIRCULAR INLETS W/ FLAT TOP

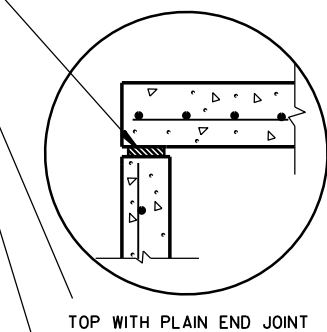


PRECAST REINFORCED CONCRETE WITH INTEGRAL BASE OPTION

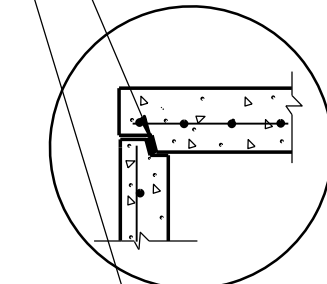


SEPARATE PRECAST REINFORCED CONCRETE BASE OPTION

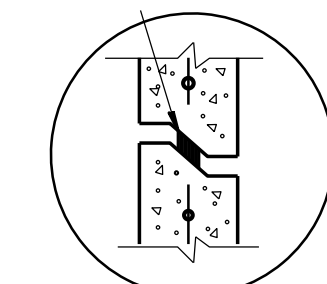
DETAIL "A"



TOP WITH PLAIN END JOINT



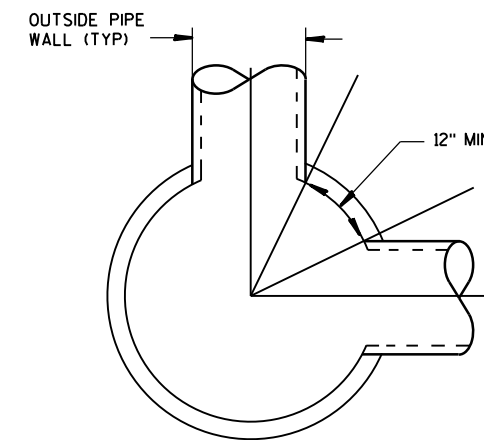
TOP WITH TONGUE AND GROOVE JOINT



RISER WITH TONGUE AND GROOVE JOINT

DETAIL "B"

INLETS 3-FT AND 4-FT DIAMETER



DETAIL "C"

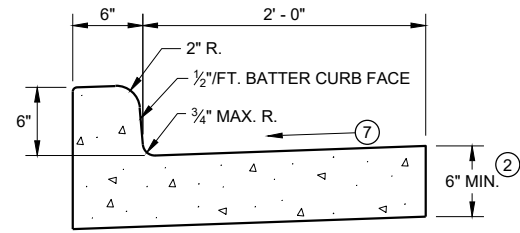
PIPE MATRIX

INLET SIZE	MAXIMUM INSIDE PIPE DIAMETER FOR TWO PIPES	
	180° SEPARATION (IN)	90° SEPARATION (IN)
3-FT	15	12
4-FT	24	18

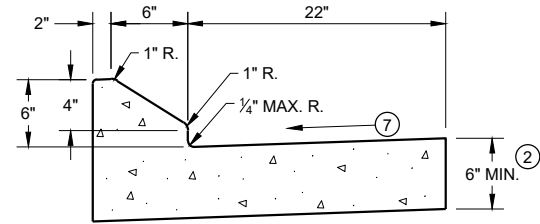
INLETS 3-FT AND 4-FT DIAMETER

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

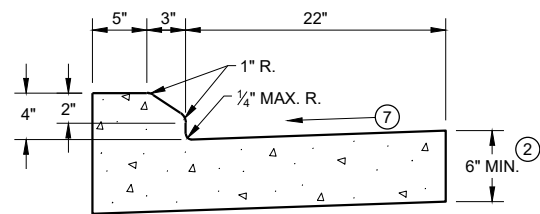
APPROVED
 Sept., 2016 /S/ Rodney Taylor
 DATE ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR
 FHWA



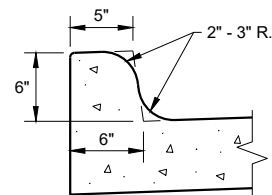
TYPES A^① & D



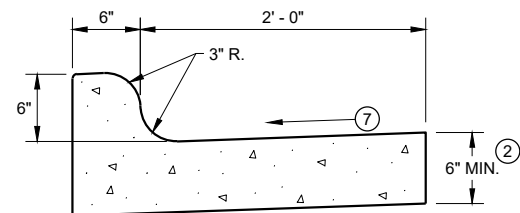
6" SLOPED CURB TYPES G^① & J



4" SLOPED CURB TYPES G^① & J

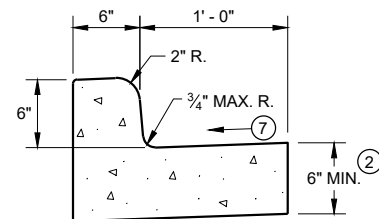


TYPES K^① & L
(OPTIONAL CURB SHAPE)



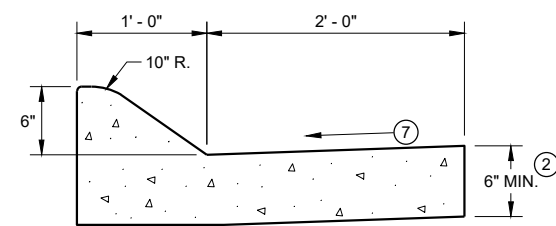
TYPES K^① & L

CONCRETE CURB AND GUTTER 30"

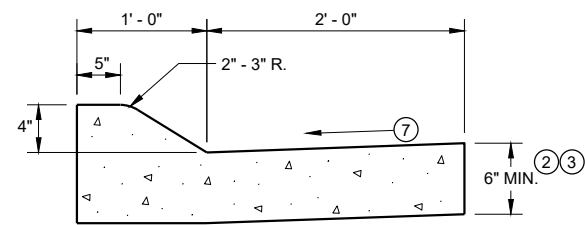


TYPES A^① & D

CONCRETE CURB AND GUTTER 18"

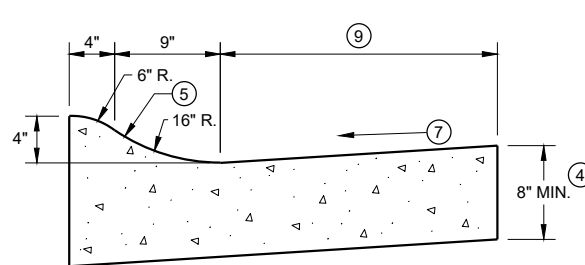


6" SLOPED CURB TYPES A^① & D



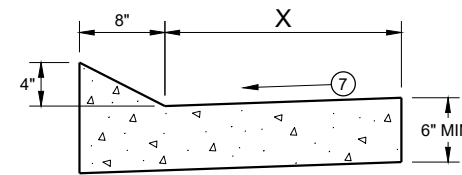
4" SLOPED CURB TYPES A^① & D

CONCRETE CURB AND GUTTER 36"



4" SLOPED CURB TYPES R^① & T

TBT & TBTT	X
30"	22"
36"	28"

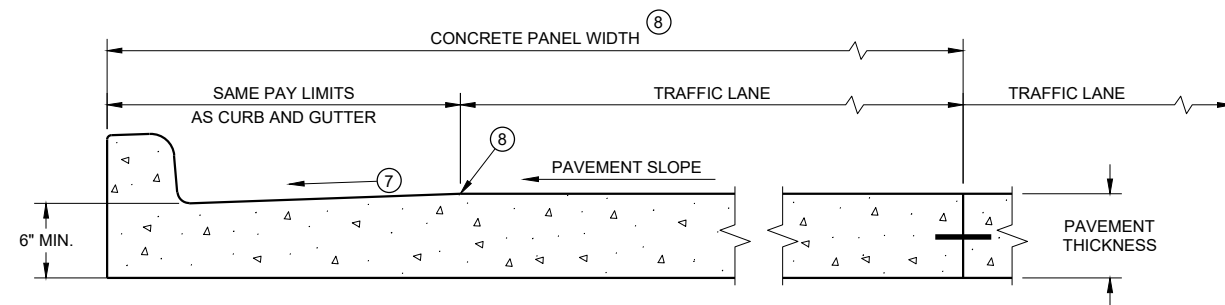


TYPES TBT & TBTT^①

CONCRETE CURB AND GUTTER

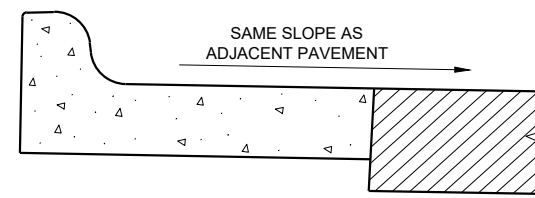
PAVEMENT THICKNESS AND MAXIMUM CONCRETE PANEL WIDTH TABLE

PAVEMENT THICKNESS	MAXIMUM PANEL WIDTH
LESS THAN 10"	12'
10" & ABOVE	15'



PARTIAL SECTION OF PAVEMENT *
WITH INTEGRAL CURB AND GUTTER

* BIKE LANE IS NOT SHOWN



REVERSE SLOPE GUTTER^⑥
(TYPICAL FOR ALL CURB & GUTTER TYPES)

GENERAL NOTES

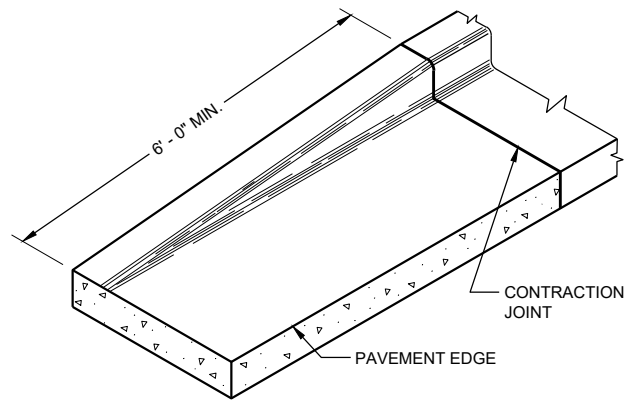
DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

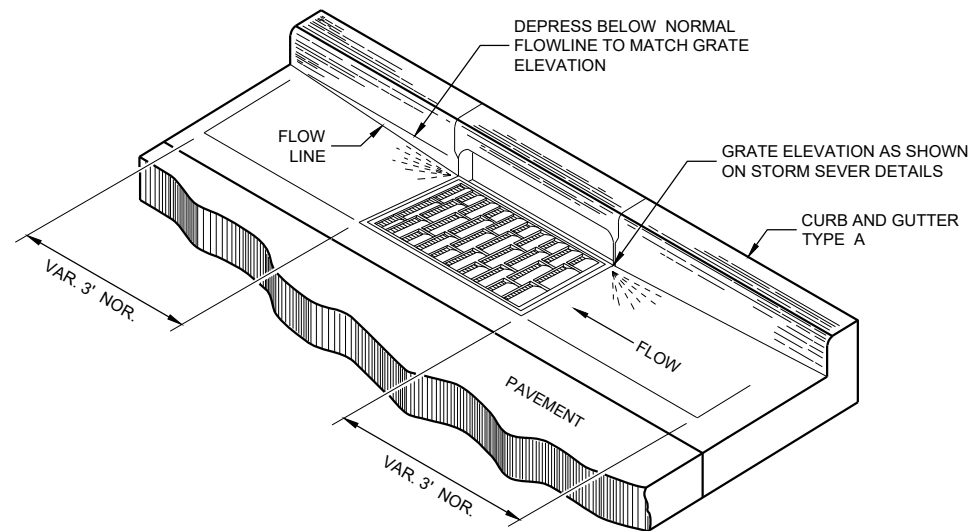
INTEGRAL CURB AND GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB AND GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ③ USE 8" MINIMUM GUTTER THICKNESS WHEN USED WITH AN ADJACENT CONCRETE TRUCK APRON PLACED BEHIND BACK OF CURB.
- ④ THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑤ UNLESS OTHERWISE NOTED, FOR STAKING PURPOSES THE FACE OF CURB IS 6" FROM THE BACK OF CURB.
- ⑥ WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.
- ⑦ USE 4% GUTTER CROSS SLOPE UNLESS OTHERWISE NOTED IN THE PLANS.
- ⑧ INCLUDE LONGITUDINAL JOINT AND TIE BARS ALONG LANE EDGE WHEN CONCRETE PANEL WIDTH EXCEEDS THE MAXIMUM WIDTH PER TABLE BELOW. LONGITUDINAL JOINT(S) ARE NOT ALLOWED WITHIN TRAFFIC LANES AND BIKE LANES. LONGITUDINAL JOINT MAY BE SAWED.
- ⑨ CONCRETE CURB AND GUTTER 4-INCH SLOPED 30-INCH TYPE "R" AND "T" = 17 INCHES
CONCRETE CURB AND GUTTER 4-INCH SLOPED 36-INCH TYPE "R" AND "T" = 23 INCHES



END SECTION CURB AND GUTTER



DETAIL OF CURB AND GUTTER AT INLETS
(TYPICAL H INLET COVER SHOWN)

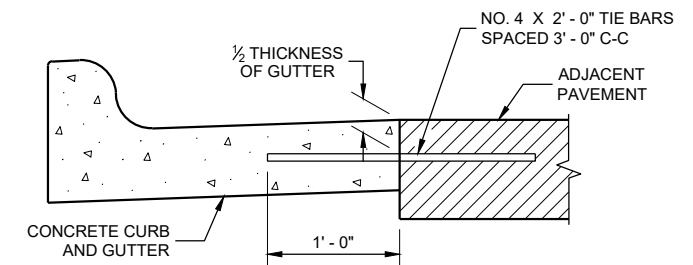
GENERAL NOTES

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

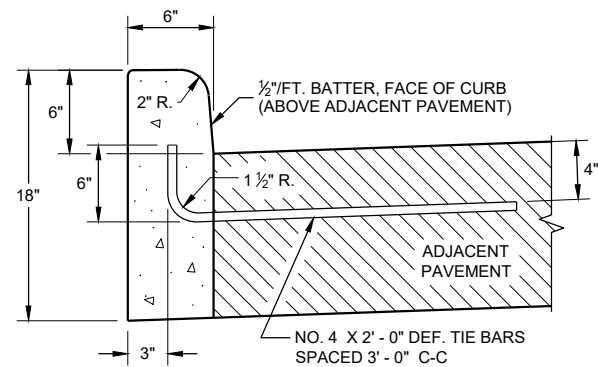
PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

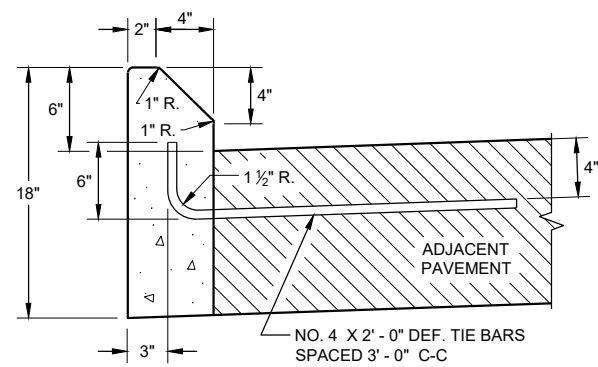
- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑨ REFER TO SDD 08D18 AND 08D19 FOR ADDITIONAL DRIVEWAY ENTRANCE CURB DETAILS.



TYPICAL TIE BAR LOCATION ①

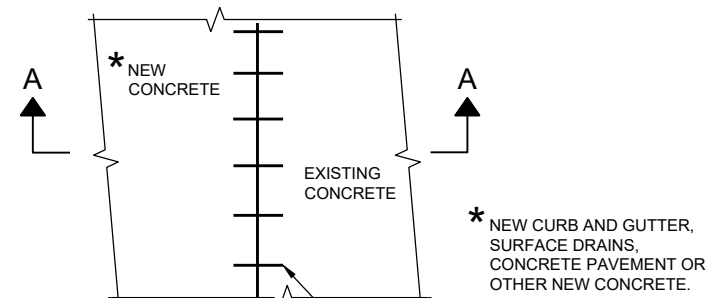


TYPES A ① & D

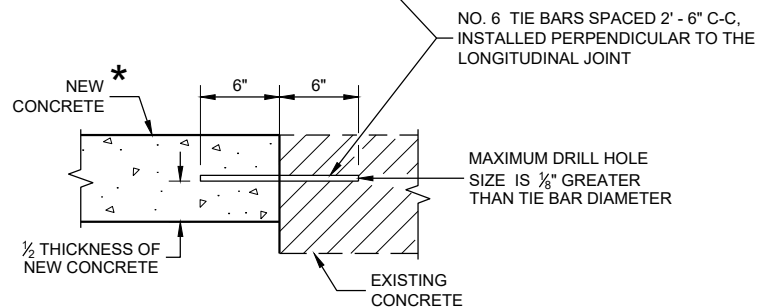


TYPES G ① & J

CONCRETE CURB

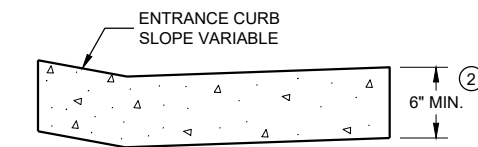


PLAN VIEW



SECTION A - A

TIE BARS DRILLED INTO EXISTING PAVEMENT



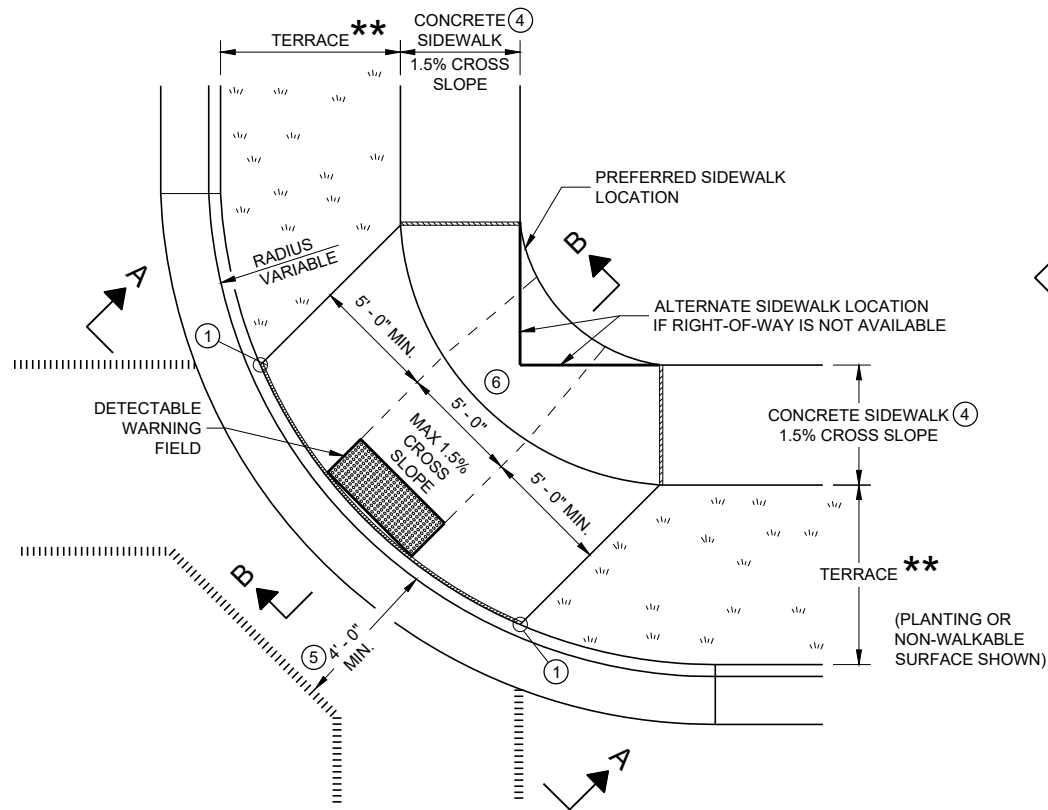
DRIVEWAY ENTRANCE CURB ⑨
(WHEN DIRECTED BY THE ENGINEER)

CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS

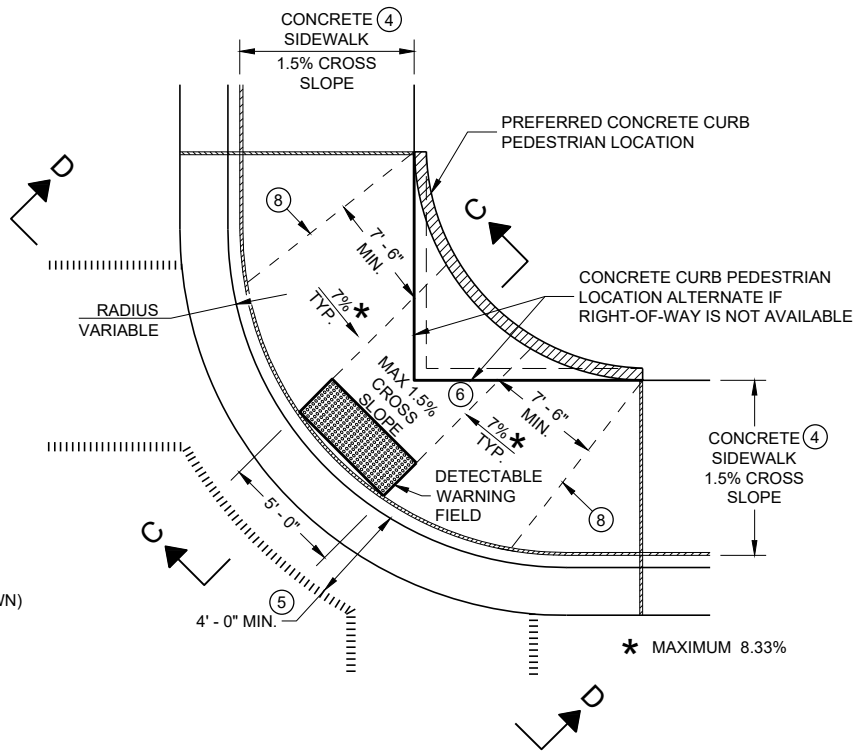
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2021 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
ENGINEER

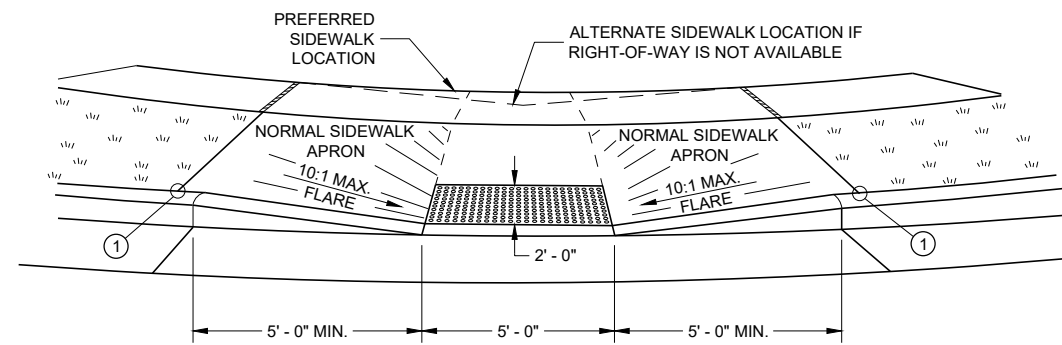
FHWA



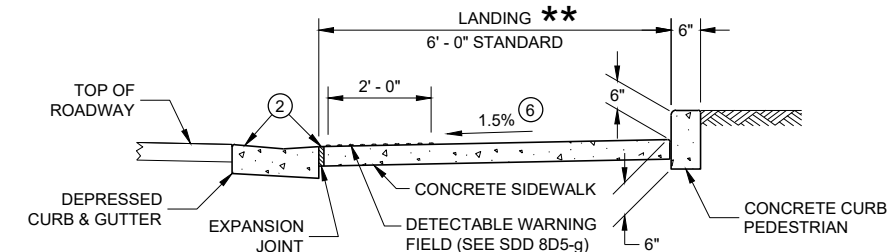
**PLAN VIEW
CURB RAMP TYPE 1
(CENTER OF CORNER RADIUS)**



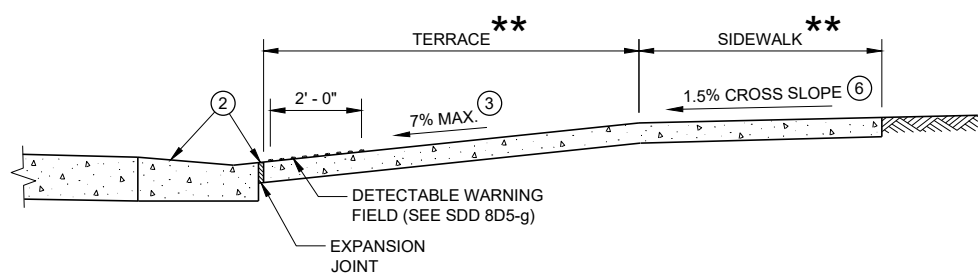
**PLAN VIEW
CURB RAMP TYPE 1 - A
(NO TERRACE)**



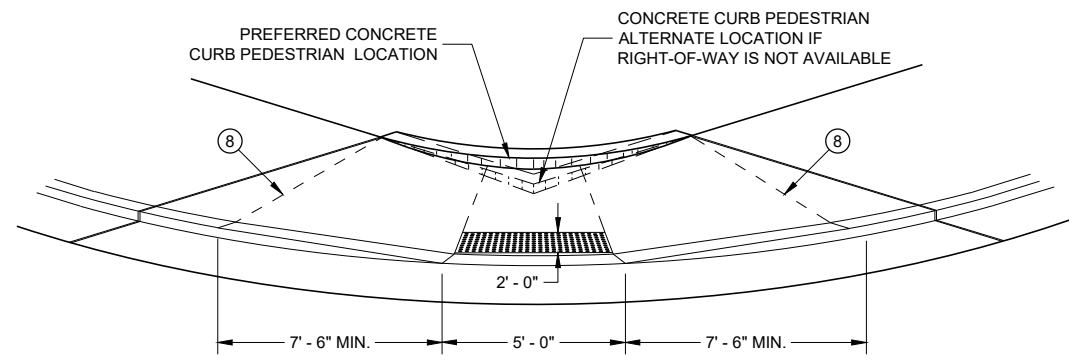
VIEW A - A FOR TYPE 1



SECTION C - C FOR TYPE 1 - A



SECTION B - B FOR TYPE 1



VIEW D - D FOR TYPE 1 - A

GENERAL NOTES

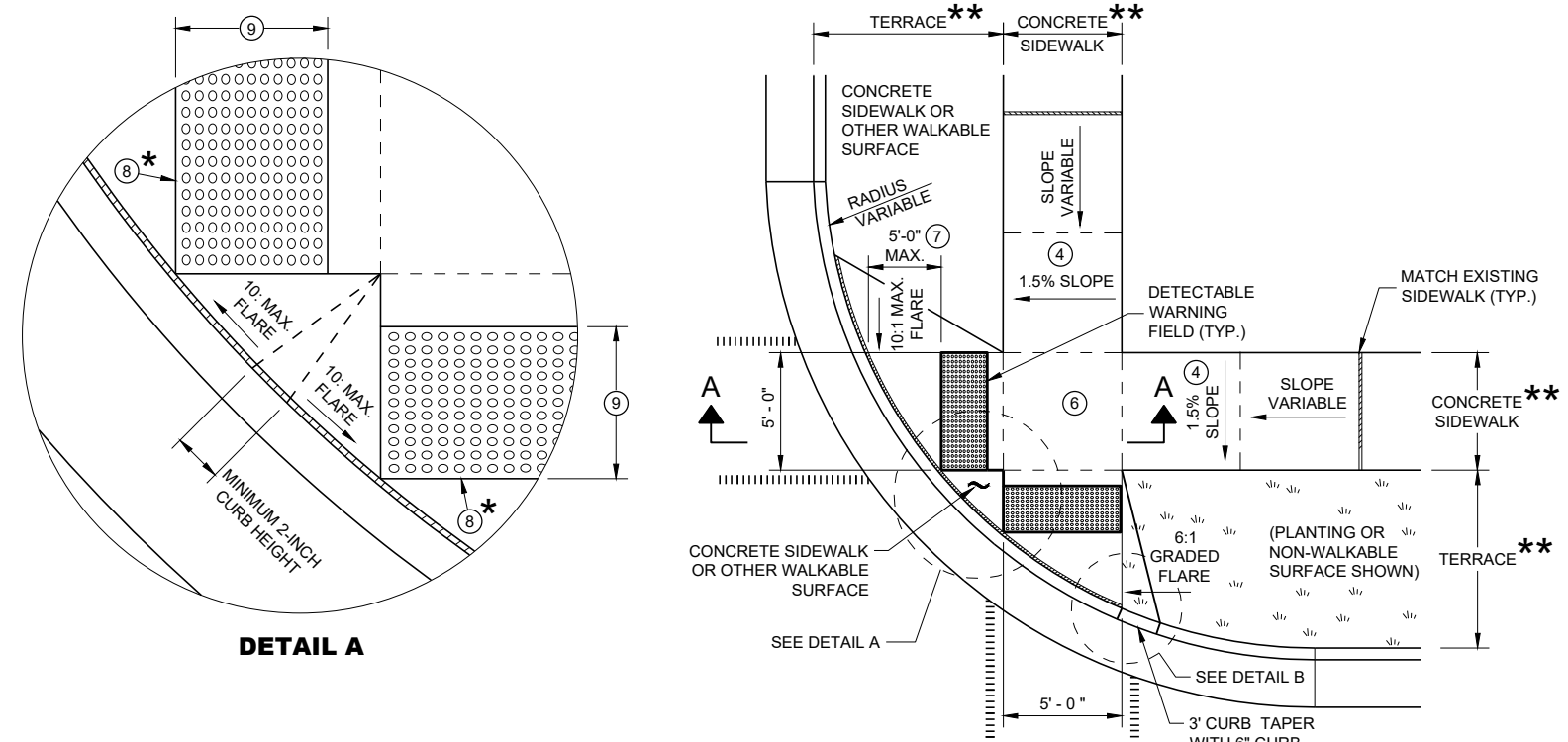
- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- WHEN NECESSARY, THE SIDEWALK ELEVATION MAY BE LOWERED TO MEET THE HIGH POINT ON THE RAMP.
- TYPE 1 CURB RAMPS SHALL HAVE A NORMAL SIDEWALK APRON AND CURB ON BOTH SIDES OF RAMP.
- DETECTABLE WARNING FIELD SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS "CURB RAMP DETECTABLE WARNING FIELD". THE CONCRETE PEDESTRIAN CURB, IF NEEDED, SHALL BE MEASURED AND PAID BY THE LINEAR FOOT AS "CONCRETE CURB PEDESTRIAN". CONCRETE SIDEWALK IN THE CURB RAMP AREA SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS CONCRETE SIDEWALK, INCLUDING THE AREA UNDER THE DETECTABLE WARNING FIELD.
- SELECT CURB RAMP DETECTABLE WARNING FIELD MATERIALS AND DEVICES FROM THE DEPARTMENT'S APPROVED MATERIALS LIST. THE COLOR OF THE DETECTABLE WARNING FIELD IS SPECIFIED ELSEWHERE AND IS INCIDENTAL TO THE BID ITEM OF "CURB RAMP DETECTABLE WARNING FIELD"
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- SURFACE TEXTURE OF THE RAMP SHALL BE OBTAINED BY COARSE BROOMING TRANSVERSE TO THE SLOPE OF THE RAMP.
- ① THIS POINT IS AN EXTENSION OF OUTSIDE EDGE OF APPROACHING SIDEWALK WHERE IT MEETS THE BACK OF CONCRETE CURB. POINT LOCATION MAY BE ADJUSTED TO ALIGN WITH BEGINNING OF FULL-HEIGHT CURB IF THIS DISTANCE IS SHORT.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4" INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ MAXIMUM 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑤ PROVIDE A LEVEL LANDING IN THE STREET AND GUTTER AREA (2% MAXIMUM SLOPE IN ANY DIRECTION). WHEN THE GUTTER SLOPE EXCEEDS 2%, CONSTRUCT THE LEVEL LANDING IN THE STREET AREA.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

LEGEND

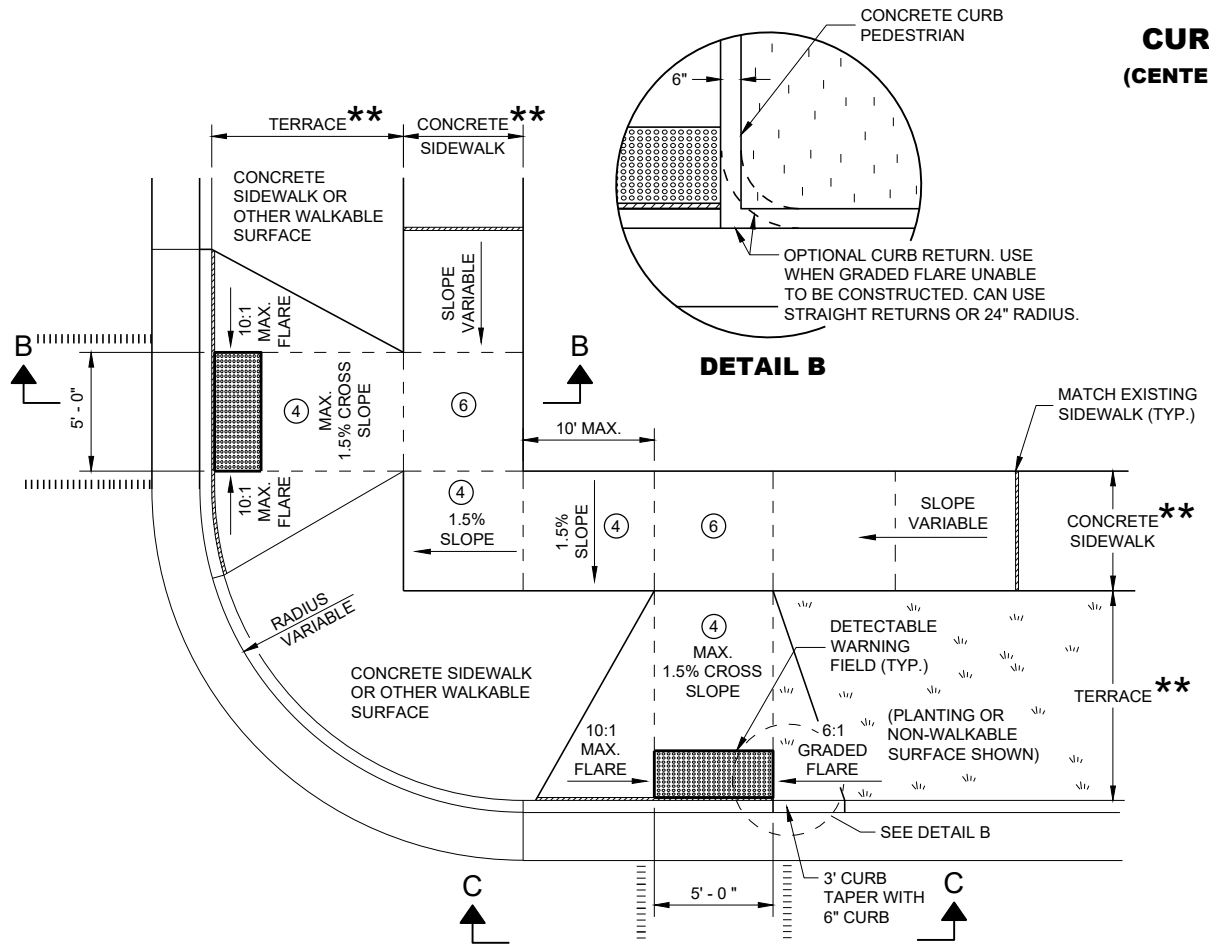
- 1/2" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)

**CURB RAMPS
TYPE 1 AND 1-A**

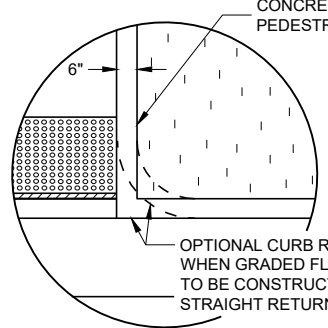
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



PLAN VIEW CURB RAMP TYPE 2 (CENTER OF CORNER RADIUS)



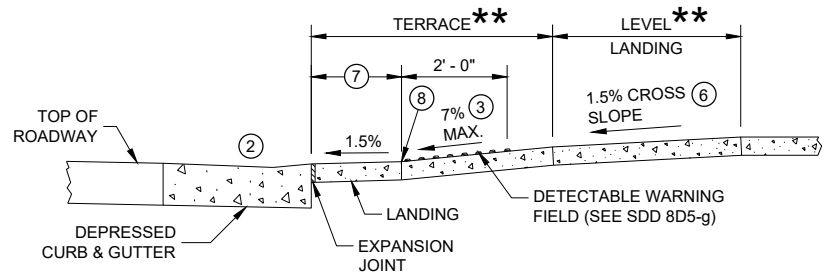
PLAN VIEW CURB RAMP TYPE 3 (OUTSIDE OF CROSSWALK AREA)



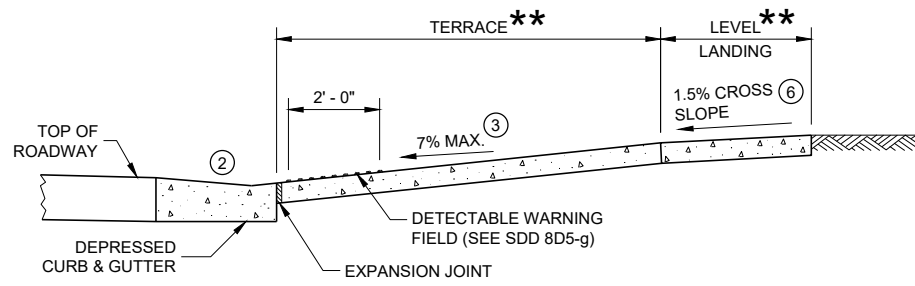
DETAIL B

GENERAL NOTES

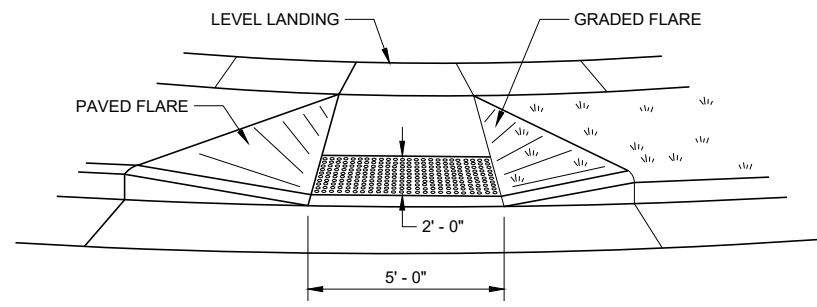
- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE (2.67% OR LESS) AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET X 5 FEET.
- ⑦ WHEN GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑨ WHEN DISTANCE IS LESS THAN 6' - 0", IT MAY BE DIFFICULT TO ACHIEVE A 7% SLOPE OR FLATTER ALONG THE RAMP. REDUCE CURB HEIGHT IN TRIANGLE AREA TO ACHIEVE 7% SLOPE OR FLATTER ON RAMP. CONSTRUCT 2-INCH MINIMUM CURB HEIGHT BETWEEN 10:1 FLARES.



SECTION A - A FOR TYPE 2



SECTION B - B FOR TYPE 3



VIEW C - C FOR TYPE 3

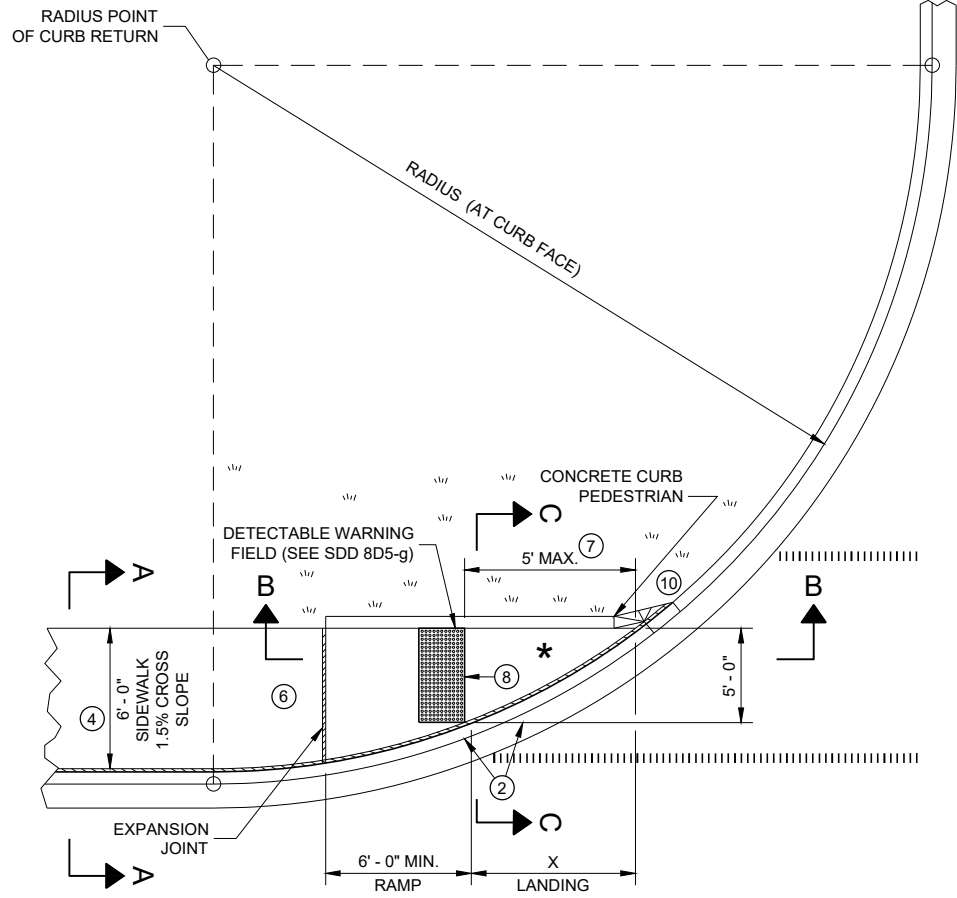
- * MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK
- ** WIDTH SHOWN ELSEWHERE IN THE PLANS

LEGEND

- 1/2" EXPANSION JOINT SIDEWALK
- - - - - CONTRACTION JOINT SIDEWALK
- ||||| PAVEMENT MARKING CROSSWALK (WHITE)

**CURB RAMPS
TYPE 2 AND 3**

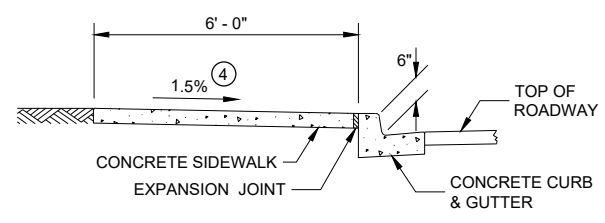
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DEPARTMENT OF TRANSPORTATION



**PLAN VIEW
CURB RAMP TYPE 4A**

RADIUS (AT CURB FACE)	X
10 FEET	4' - 7"
15 FEET	6' - 5 1/2"

INTERMEDIATE RADII CAN BE INTERPOLATED



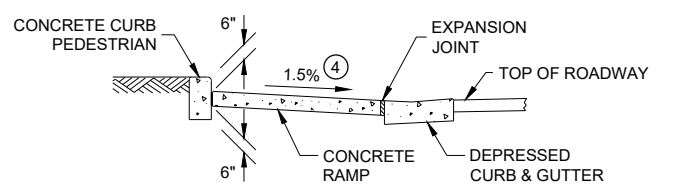
SECTION A - A FOR TYPE 4A

GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4" INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑦ WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑩ INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

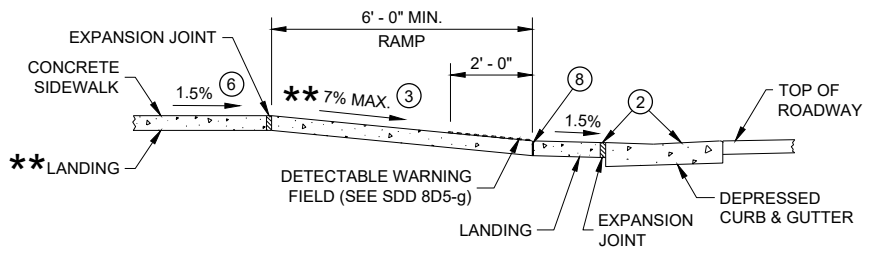
LEGEND

- 1/2" EXPANSION JOINT SIDEWALK
- - - CONTRACTION JOINT SIDEWALK
- ||||| PAVEMENT MARKING CROSSWALK (WHITE)



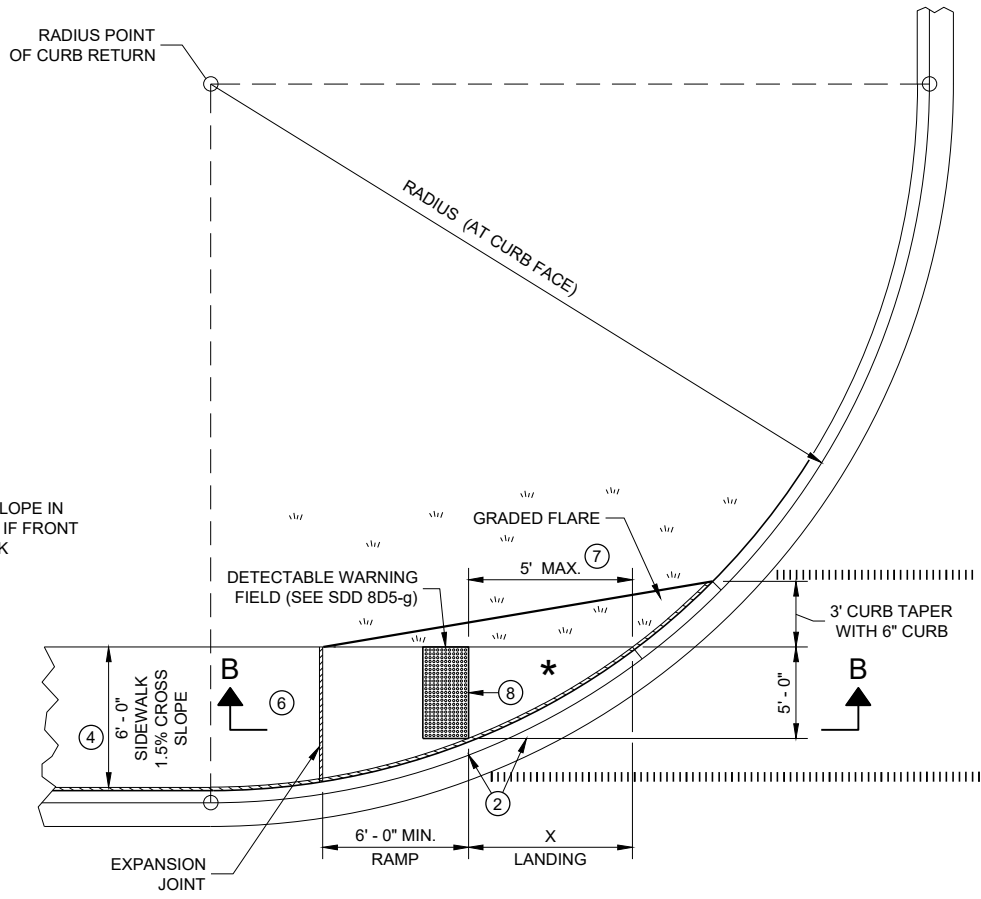
SECTION C - C FOR TYPE 4A

* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IF FRONT OF GRADE BREAK

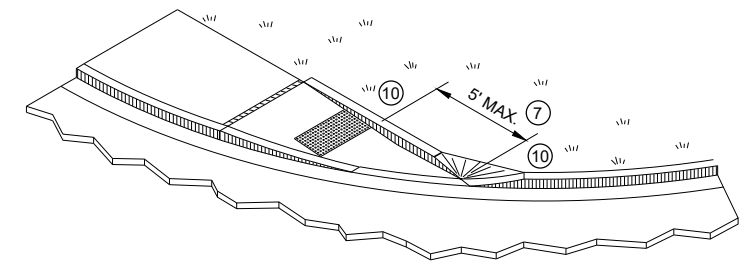


**SECTION B - B FOR
TYPE 4A AND TYPE 4A1**

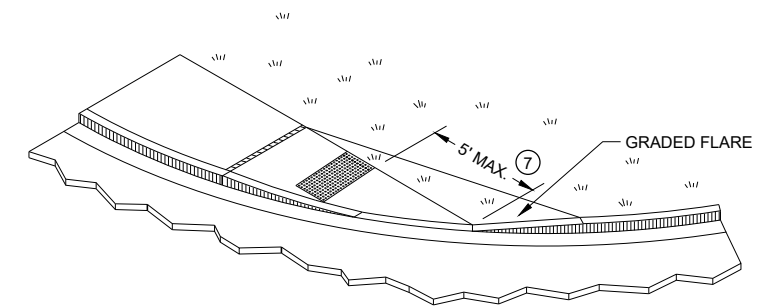
** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED



**PLAN VIEW
CURB RAMP TYPE 4A1**



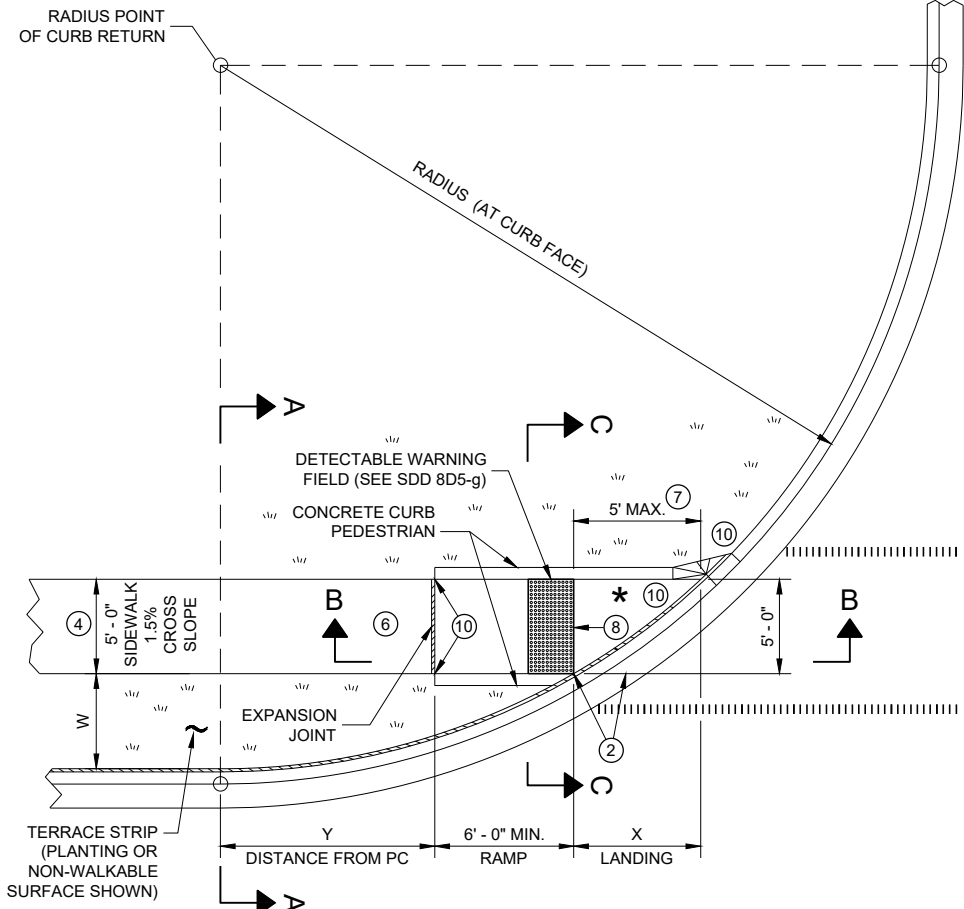
ISOMETRIC VIEW FOR TYPE 4A



ISOMETRIC VIEW FOR TYPE 4A1

**CURB RAMPS
TYPE 4A AND 4A1**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



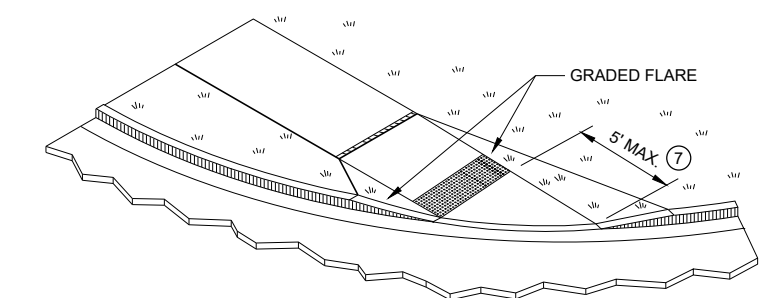
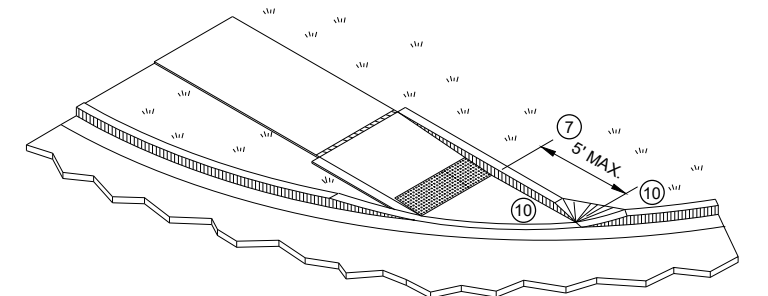
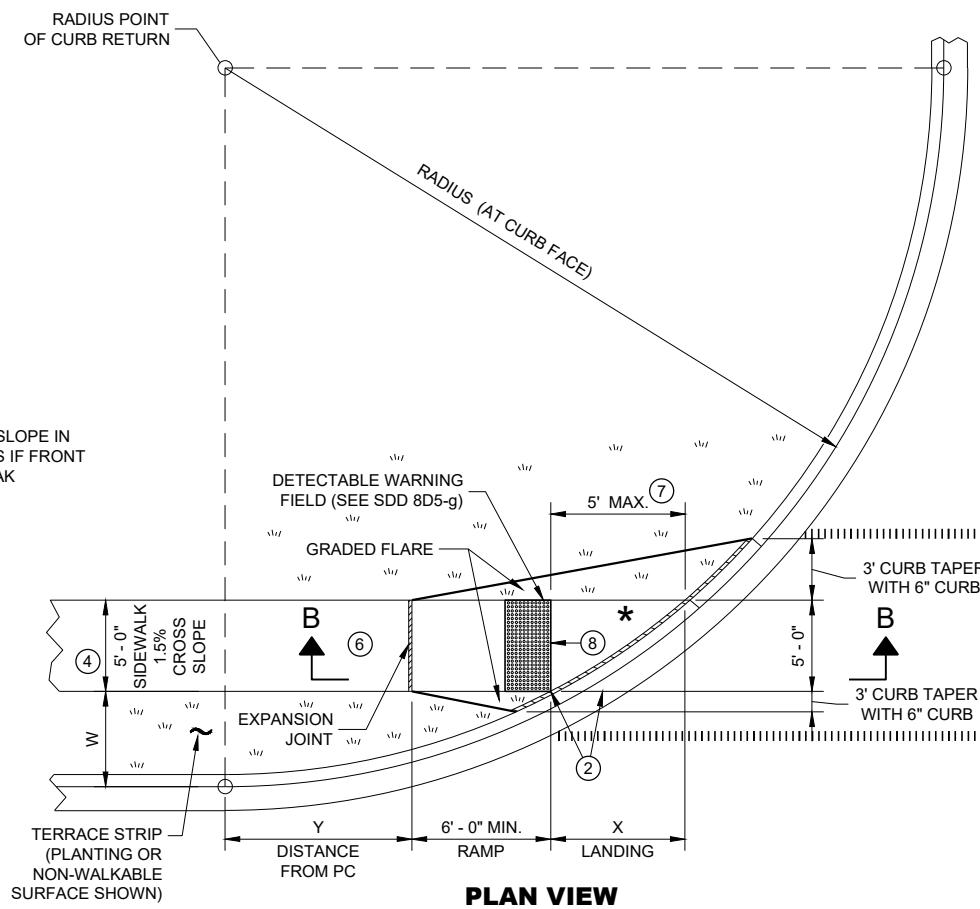
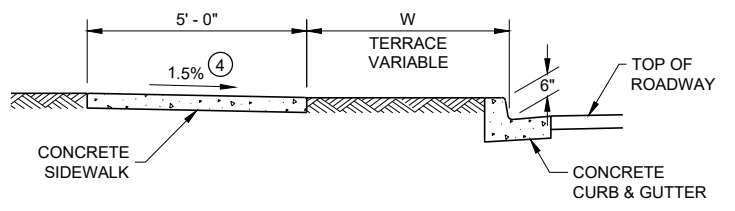
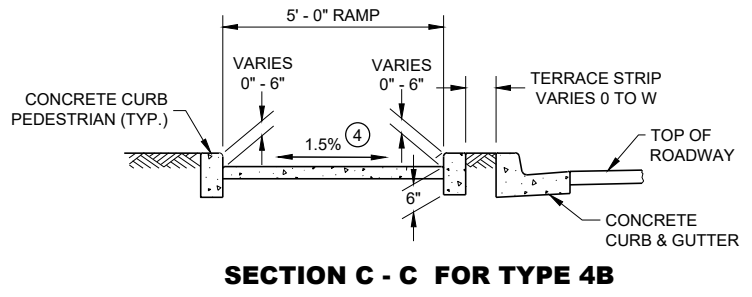
RADIUS (AT CURB FACE)	W = 3' - 0"		W = 4' - 0"		W = 5' - 0"		W = 6' - 0"		W = 7' - 0"		W = 8' - 0"		W = 9' - 0"		W = 10' - 0"	
	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y
10 FEET	2' - 10 1/4"	0' - 5"	2' - 1"	1' - 4 1/2"	1' - 5"	2' - 1"	0' - 10"	2' - 7 1/2"	0' - 3 1/4"	3' - 0 1/4"						
15 FEET	4' - 6 3/4"	2' - 1 3/4"	3' - 9"	3' - 5 3/4"	3' - 1 1/4"	4' - 6"	2' - 6 3/4"	5' - 4 1/2"	2' - 1"	6' - 1"	1' - 8"	6' - 8 1/2"	1' - 3 1/4"	7' - 2 1/2"	0' - 10 3/4"	7' - 7 1/4"
20 FEET	5' - 9 3/4"	3' - 6 1/2"	4' - 11 1/2"	5' - 1 3/4"	4' - 3 3/4"	6' - 5 1/2"	3' - 8 3/4"	7' - 7"	3' - 3"	8' - 6 1/2"	2' - 10"	9' - 4 1/2"	2' - 5 1/2"	10' - 1 1/4"	2' - 1 1/4"	10' - 9"
30 FEET			6' - 9 1/4"	7' - 11 1/4"	6' - 0 1/4"	9' - 8"	5' - 5"	11' - 1 3/4"	4' - 10 3/4"	12' - 5 3/4"	4' - 5 1/2"	13' - 7 3/4"	4' - 0 3/4"	14' - 8 1/2"	3' - 8 1/2"	15' - 8 1/4"
40 FEET									6' - 1 3/4"	15' - 8 1/2"	5' - 8"	17' - 2"	5' - 3"	18' - 5 3/4"	4' - 10 3/4"	19' - 8 1/4"
50 FEET															5' - 10 1/4"	23' - 2"

INTERMEDIATE RADII CAN BE INTERPOLATED
 DIMENSION "Y" IS CALCULATED BASED ON 6'-0" RAMP LENGTH
 DIMENSION "X" IS CALCULATED BASED ON 5'-0" SIDEWALK WIDTH

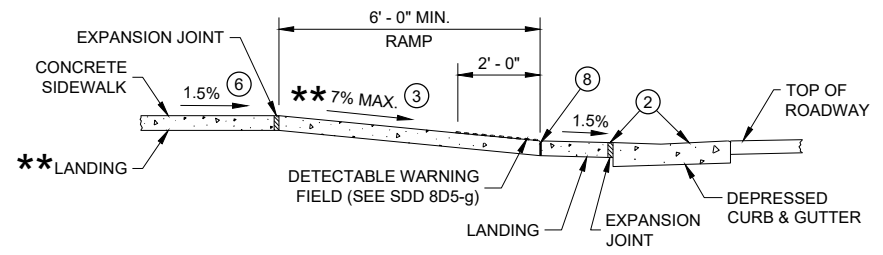
- LEGEND**
- ===== 1/2" EXPANSION JOINT SIDEWALK
 - - - - - CONTRACTION JOINT SIDEWALK
 - ||||||| PAVEMENT MARKING CROSSWALK (WHITE)

GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/8" INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑦ WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑩ INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.



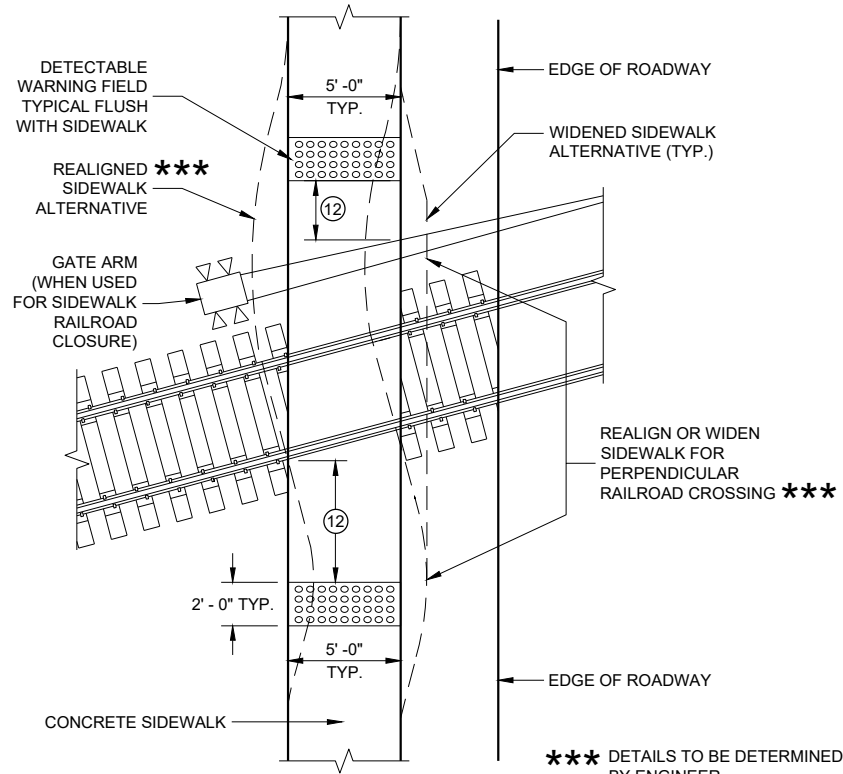
* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IF FRONT OF GRADE BREAK



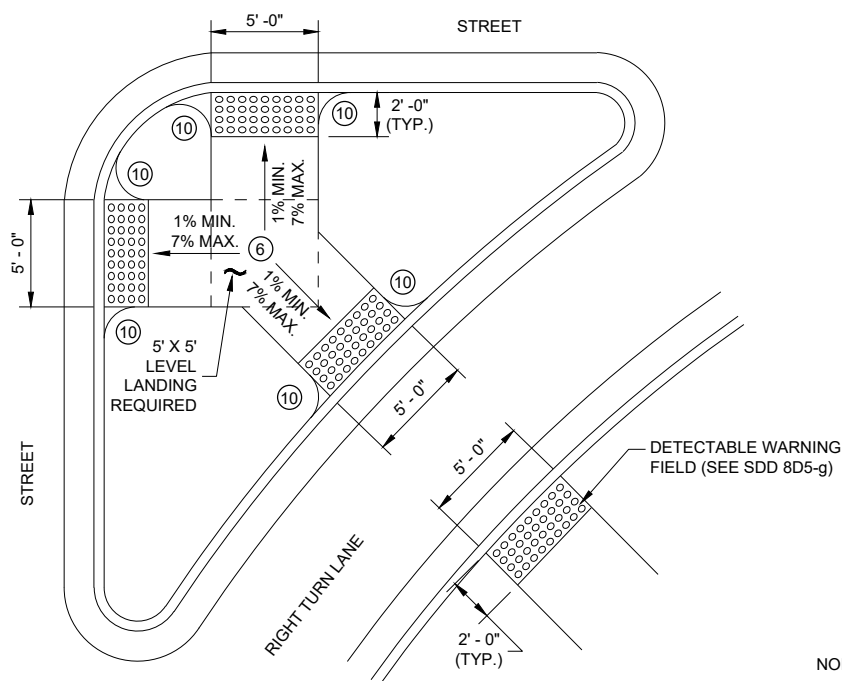
** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

**CURB RAMPS
TYPE 4B AND 4B1**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

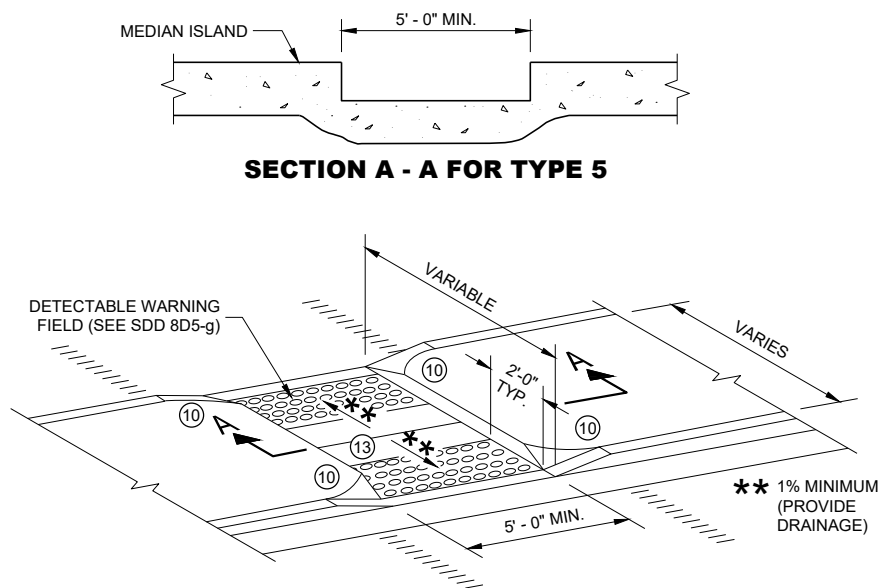


CURB RAMP TYPE 8
DETECTABLE WARNINGS
AT RAILROAD CROSSING

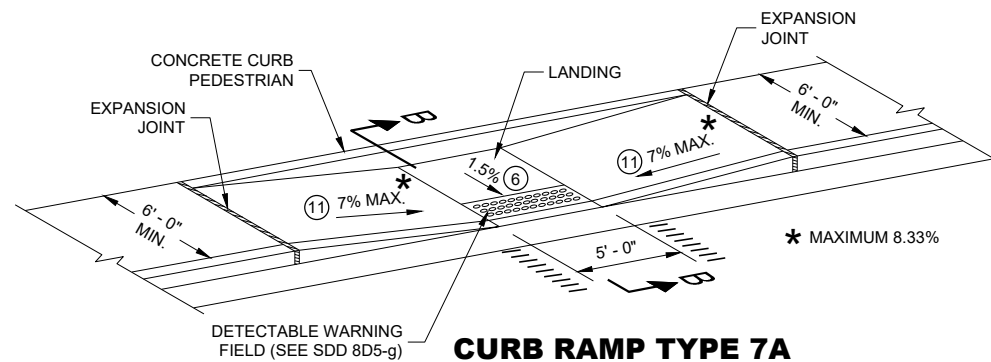


CURB RAMP TYPE 6
DETECTABLE WARNING AT ISLANDS

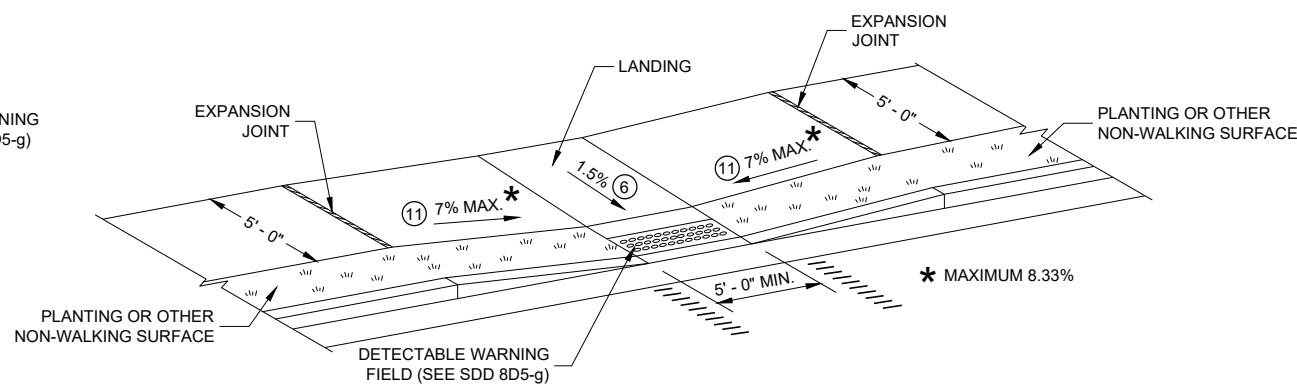
REFER TO GENERAL NOTES (2) AND (3) FOR ALL ISLAND CURB RAMPS



CURB RAMP TYPE 5
MEDIAN ISLAND
NON-ELEVATED PEDESTRIAN CROSSING



CURB RAMP TYPE 7A
MID BLOCK CROSSING



CURB RAMP TYPE 7B
MID BLOCK CROSSING

NOTE: THESE PARALLEL AND PARALLEL/PERPENDICULAR CURB RAMPS MAY BE USED AT INTERSECTIONS AND MID BLOCK LOCATIONS.

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

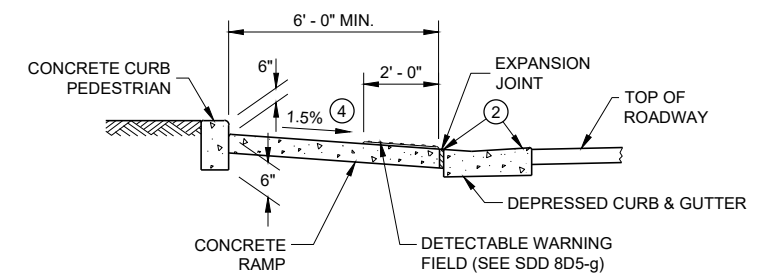
SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

- (2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- (3) AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- (4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- (6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- (10) INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.
- (11) SLOPE SIDEWALK TOWARD LANDING AS SHOWN WHERE THERE IS NO TERRACE OR WHERE THE TERRACE WIDTH IS LESS THAN 6 FEET WIDE.
- (12) THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO A RAILROAD CROSSING SHALL BE 1.5 FEET ±0.1' FROM THE FACE OF THE GATE ARM IF THE GATE ARM EXTENDS ACROSS THE SIDEWALK. WHERE THERE IS NO PEDESTRIAN GATE, THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO THE RAILROAD CROSSING SHALL BE 15 FEET FROM THE NEAREST RAIL.
- (13) DO NOT INSTALL DETECTABLE WARNING FIELDS AT THE EDGES OF STREET-LEVEL PEDESTRIAN REFUGE ISLANDS IF A MINIMUM 2 FOOT CONCRETE SURFACE WITHOUT DETECTABLE WARNINGS (MEASURED IN THE DIRECTION OF PEDESTRIAN TRAVEL) CANNOT BE ACHIEVED.

LEGEND

- ===== 1/2" EXPANSION JOINT SIDEWALK
- - - - - CONTRACTION JOINT FIELD LOCATED
- ||||||| PAVEMENT MARKING CROSSWALK (WHITE)

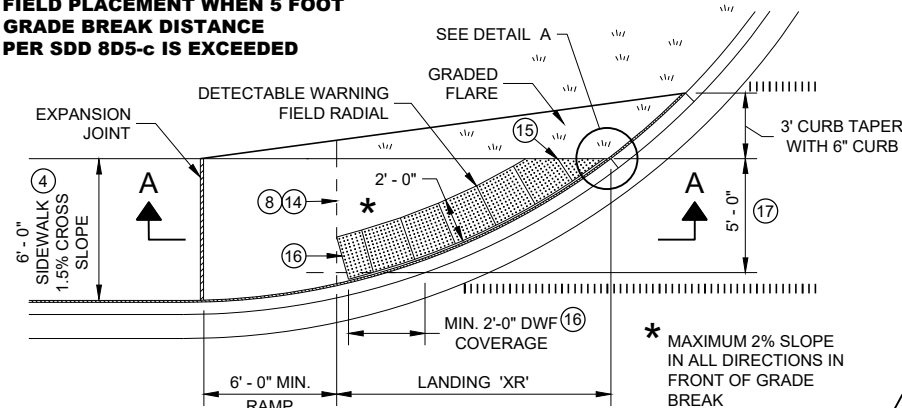


SECTION B - B FOR TYPE 7A

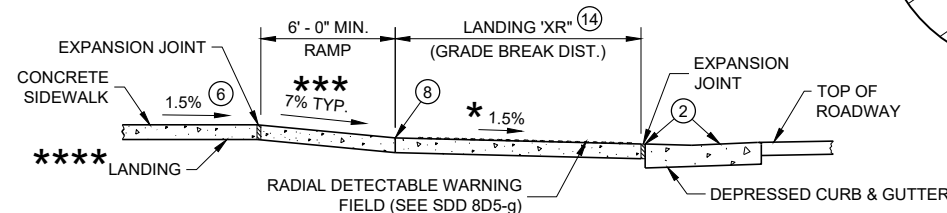
CURB RAMPS
TYPE 5, 6, 7A, 7B & 8

STATE OF WISCONSIN
 DEPARTMENT OF TRANSPORTATION

RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-c IS EXCEEDED



PLAN VIEW CURB RAMP TYPE 4A1 (GRADE BREAK DISTANCE GREATER THAN 5 FEET)

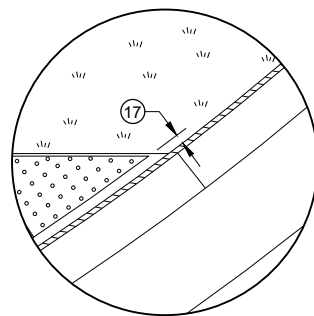


SECTION A - A FOR TYPE 4A1

**** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

*** MAXIMUM 8.33%

- LEGEND**
- 1/2" EXPANSION JOINT SIDEWALK
 - - - - CONTRACTION JOINT SIDEWALK
 - ||||| PAVEMENT MARKING CROSSWALK (WHITE)

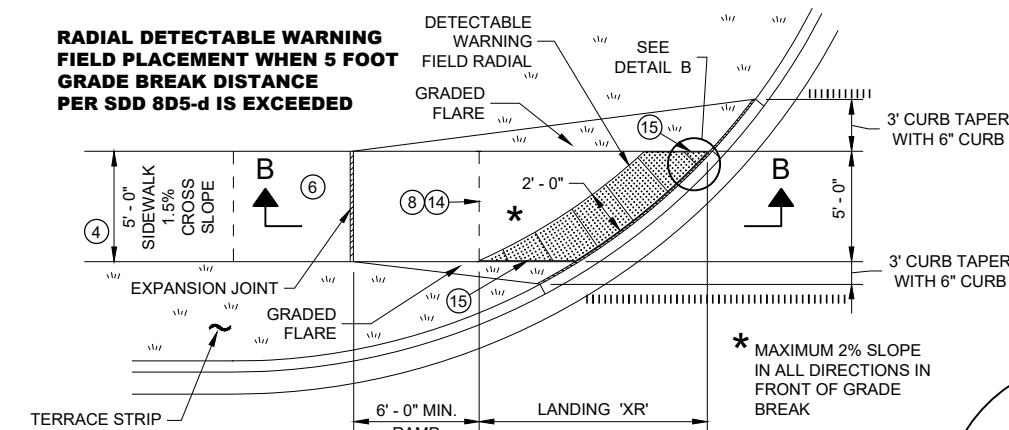


DETAIL A

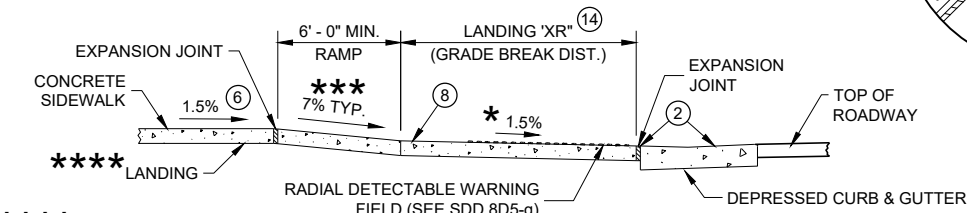
GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- APPLY RADIAL DETECTABLE WARNING PLACEMENT SIMILARLY FOR TYPE 4A AND 4A1 CURB RAMPS AND SIMILARLY FOR TYPE 4B AND 4B1 CURB RAMPS. TYPE 4A AND 4B RAMPS ARE NOT SHOWN.
- REFER TO SDD 8D5-g FOR ADDITIONAL RADIAL PLATE REQUIREMENTS.
- FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FILED ARE PROHIBITED.
- DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.
- 2 GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- 3 AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- 4 ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- 6 PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET BY 5 FEET.
- 8 PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- 14 CONSULT ENGINEER IF GRADE BREAK LOCATION (END OF LANDING DIMENSION "XR") REQUIRES FIELD ADJUSTMENT WHEN ESTABLISHING FINAL RADIAL DETECTABLE WARNING FIELD LOCATION.
- 15 FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN 1/8" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.
- 16 USE 1' X 2" RECTANGULAR END PLATE AT END OF TYPE 4A1 RAMP AND PROVIDE MINIMUM 2' - 0" DETECTABLE WARNING FIELD COVERAGE (IN DIRECTION OF PEDESTRIAN TRAVEL) ALONG THE ENTIRE CURB RAMP WIDTH.
- 17 A MAXIMUM 3 INCH CONCRETE BORDER WITH IS ALLOWABLE IN FROM OF RADIAL DETECTABLE WARNING FIELD FOR CONSTRUCTABILITY PURPOSES. CONCRETE BORDER WIDTH MAY VARY UP TO 1 INCH.

RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-d IS EXCEEDED



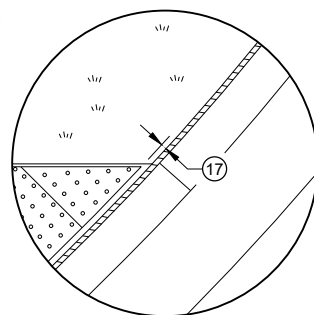
PLAN VIEW CURB RAMP TYPE 4B1 (GRADE BREAK DISTANCE GREATER THAN 5 FEET)



SECTION B - B FOR TYPE 4B1

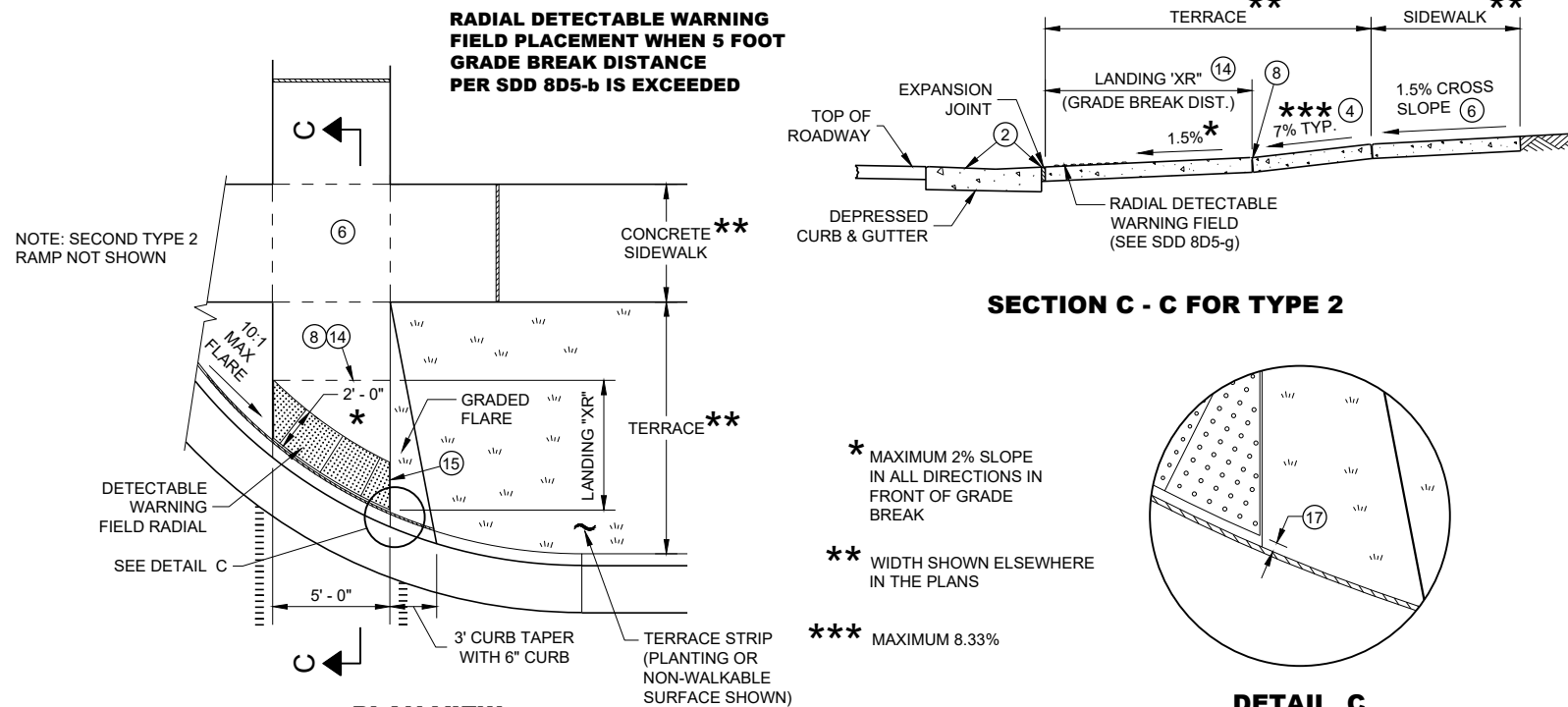
**** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

*** MAXIMUM 8.33%



DETAIL B

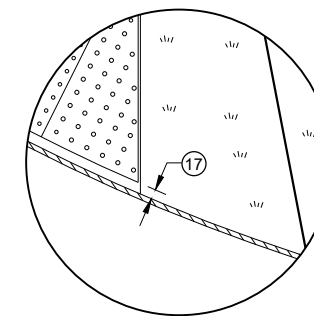
RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-b IS EXCEEDED



PLAN VIEW CURB RAMP TYPE 2 (GRADE BREAK DISTANCE GREATER THAN 5 FEET) (ON LINE WITH SIDEWALK)

NOTE: SECOND TYPE 2 RAMP NOT SHOWN

- * MAXIMUM 2% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK
- ** WIDTH SHOWN ELSEWHERE IN THE PLANS
- *** MAXIMUM 8.33%



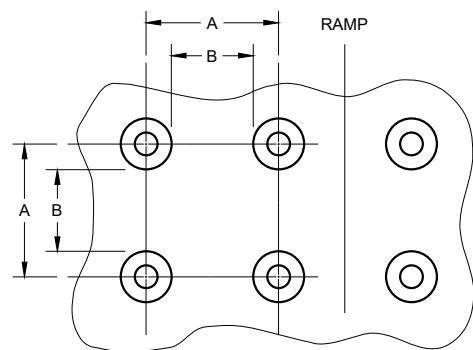
DETAIL C

CURB RAMPS RADIAL DETECTABLE WARNING FIELD APPLICATIONS

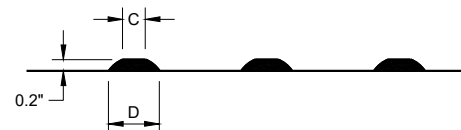
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

	MIN.	MAX.
A	1.6"	2.4"
B	0.65"	1.5"
C	*	*
D	0.9"	1.4"

* THE C DIMENSION IS 50% TO 65% OF THE D DIMENSION.

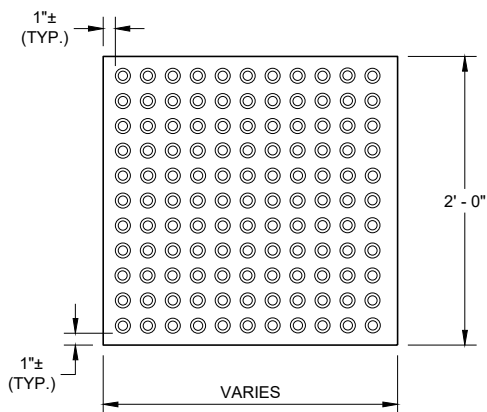


PLAN VIEW

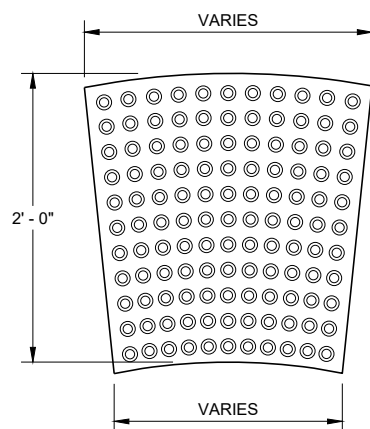


ELEVATION VIEW

**TRUNCATED DOMES
DETECTABLE WARNING PATTERN DETAIL**

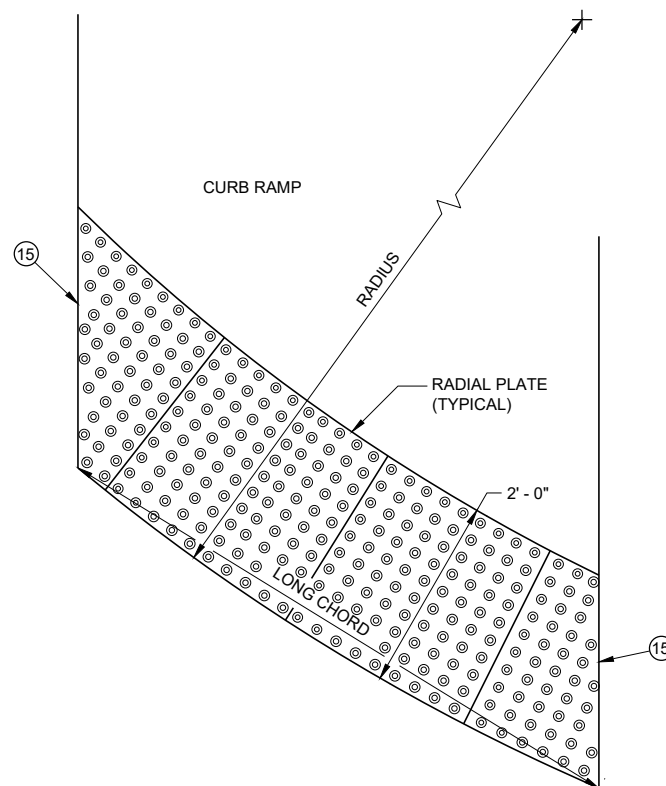


RECTANGULAR
PLATES

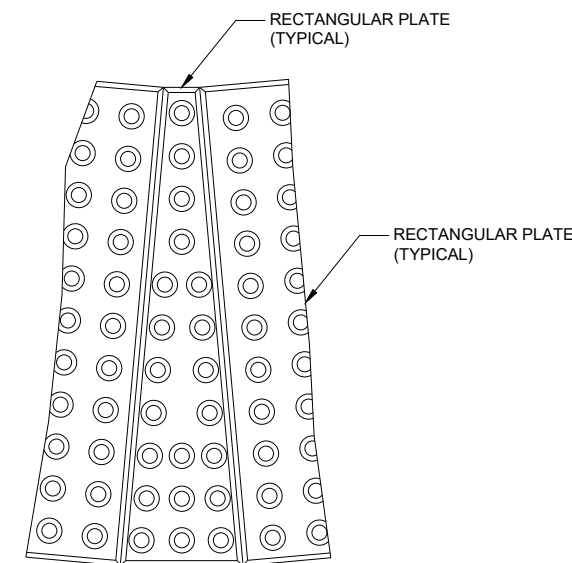


RADIAL
PLATES

PLAN VIEW
DETECTABLE WARNING FIELDS (TYPICAL)



PLAN VIEW
RADIAL DETECTABLE
WARNING FIELD ATTRIBUTES



PLAN VIEW
RADIAL WEDGE PLATE
CONNECTION DETAIL

GENERAL NOTES

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AT A CURB RAMP SHALL BE FROM THE SAME MANUFACTURER.

PLACE ALL DETECTABLE WARNING FIELD SYSTEMS IN ACCORDANCE TO THE MANUFACTURER'S RECOMMENDATION.

FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FIELD ARE PROHIBITED.

DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.

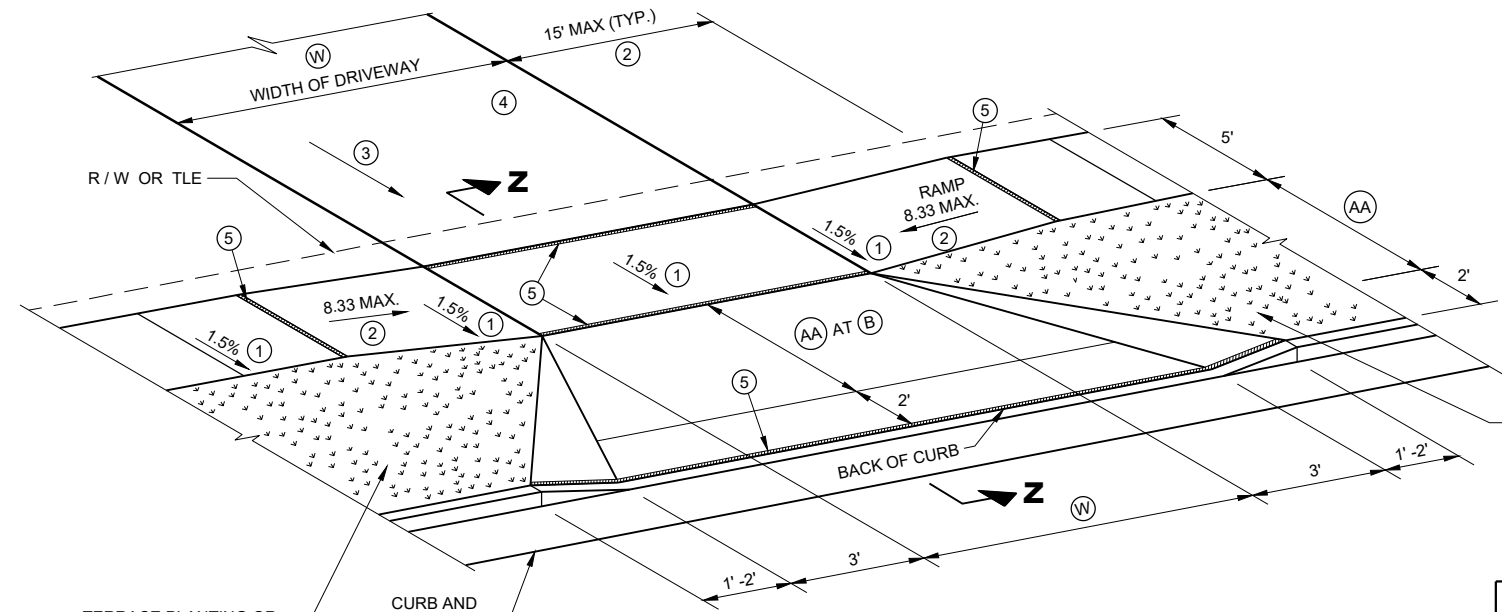
FOR RADIAL DETECTABLE WARNING FIELD APPLICATIONS WHERE STANDARD RADIAL PLATES ARE NOT AVAILABLE AT AN INTERSECTION CURB RADIUS, A COMBINATION OF SQUARE OR RECTANGULAR PLATES AND RADIAL PLATES MAY BE USED TO FORM RADIAL CONFIGURATION. RADIAL WEDGE PLATES IN COMBINATION WITH SQUARE PLATES ARE ALSO ACCEPTABLE. FOLLOW MANUFACTURER'S RECOMMENDATIONS.

REFER TO CONTRACT AND STANDARD SPECIFICATIONS FOR FIELD CUTTING REQUIREMENTS.

DO NOT EMBED IN CONCRETE ANY FIELD-CUT PLATES WITH CUT EDGES SHORTER THAN 6 INCHES. CONSULT WITH MANUFACTURER FOR RE-DRILLING AND ANCHORING REQUIREMENTS OF FIELD-CUT PLATES.

15 FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN 1/8" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.

CURB RAMPS RECTANGULAR AND RADIAL DETECTABLE WARNING PLATES	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2019 DATE	/S/ Rodney Taylor ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR
<small>FHWA</small>	



TYPE Z
SIDEWALK WITH WIDER TERRACE
TERRACE VARIES 7 TO 12 FEET

GENERAL NOTES

PROVIDE CONSTRUCTION JOINTS ALONG THE CENTER OF THE CONCRETE FOR DRIVEWAYS UNDER 20 FEET IN WIDTH AND AT THE THIRD POINTS OVER 20 FEET IN WIDTH.

(W) IS SHOWN ON PLAN AND PROFILE SHEETS.

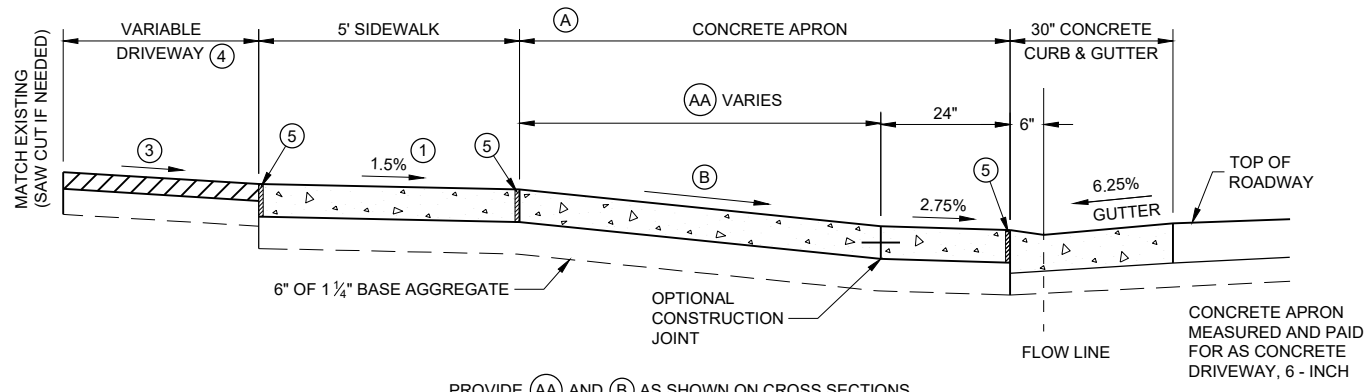
OFFSETS, ELEVATIONS, AND PERCENT GRADE ARE SHOWN ON THE CROSS SECTIONS.

- ① CONSTRUCTION TOLERANCE OF 0.5%± FOR SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.
- ② THE SIDEWALK RAMP MAXIMUM RUNNING SLOPE SHALL NOT REQUIRE THE RAMP LENGTH TO EXCEED 15 FEET TO AVOID CHASING THE SLOPE INDEFINITELY WHEN CONNECTING TO STEEP GRADES. WHEN APPLYING THE 15 FOOT MAXIMUM LENGTH, THE RUNNING SLOPE OF THE SIDEWALK SHALL BE AS FLAT AS FEASIBLE AND NOT EXCEED THE LONGITUDINAL GRADE OF THE ROADWAY.
- ③ DRIVEWAY SLOPES: DESIRABLE MAXIMUM
 10.5% UP AWAY FROM SIDEWALK (SAG)
 8.5% DOWN AWAY FROM SIDEWALK (CREST)
 ABSOLUTE MAXIMUM 15% FOR BOTH CREST AND SAG
- ④ DRIVEWAY TYPES
 · 6-INCH CONCRETE DRIVEWAY PAVEMENT OVER 6-INCH BASE AGGREGATE
 · 2-INCH TO 3-INCH ASPHALTIC SURFACE OVER 6-INCH BASE AGGREGATE
 · 6-INCH BASE AGGREGATE (MAY BE INCREASED FOR CLAY SUBGRADES.)
- ⑤ ½" EXPANSION JOINT FILLER.

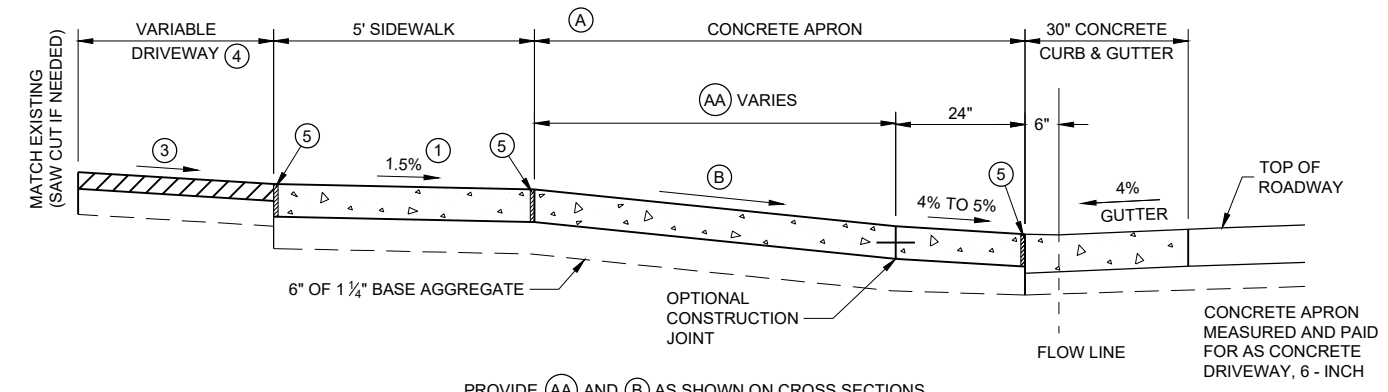
TABLE Z

(AA) FEET	(B) % 6.25% GUTTER	(B) % 4% GUTTER
4.5'	11.5%	9% TO 11.5%
5.5'	9% TO 11.5%	8% TO 11.5%
6.5'	8% TO 11.5%	6% TO 11.5%
7.5'	7% TO 11.5%	6% TO 11.5%
8.5'	6% TO 11.5%	5% TO 11.5%
9.5'	5% TO 11.5%	4% TO 11.5%

(W): 12' MIN. - 24' MAX. RESIDENTIAL AND NON-COMMERCIAL (PE & FE)
 16' MIN. - 35' MAX. COMMERCIAL (CE)



6.25% GUTTER SLOPE



4% GUTTER SLOPE

NOTE: SIDEWALK MAY BE DEPRESSED IN DRIVEWAY AREAS FOR (B) VALUES NOT SHOWN IN TABLE Z.
 SIDEWALK WITHIN THE LIMITS OF THE DRIVEWAY PAID FOR AS CONCRETE DRIVEWAY 6-INCH.
 SEPARATE PAYMENT FOR BASE AGGREGATE WILL BE MADE.

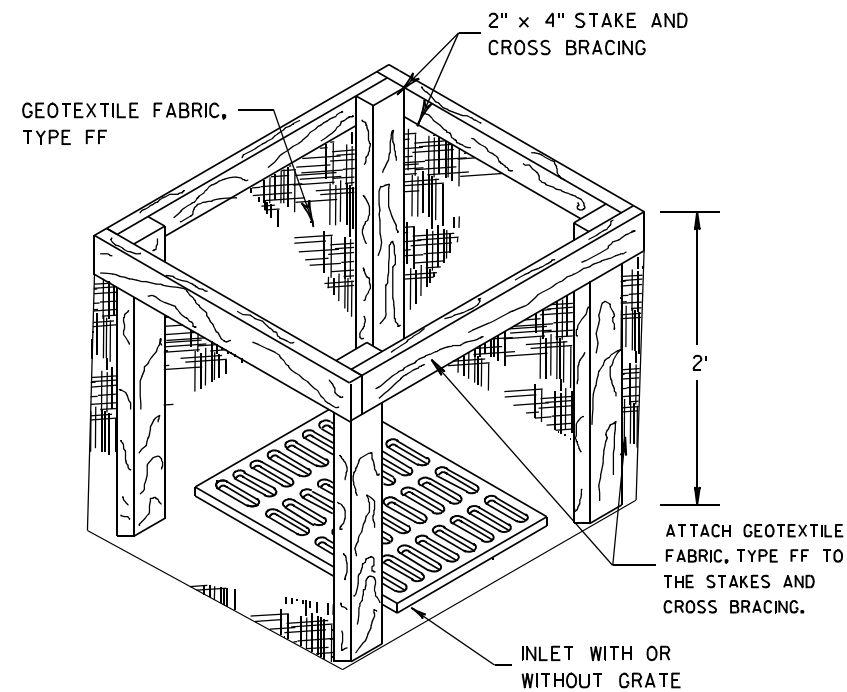
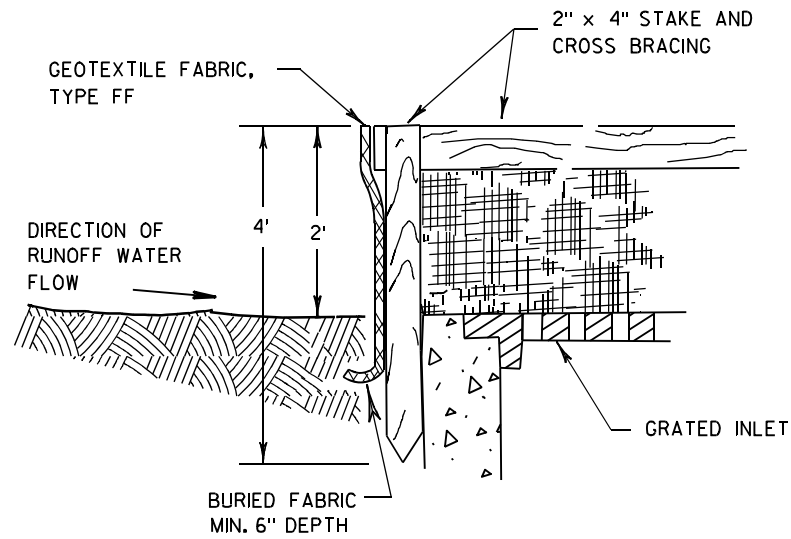
SECTION Z - Z
DRIVEWAY DETAIL WITH CONCRETE CURB AND GUTTER
(URBAN AND SUBURBAN)

DRIVEWAY AND SIDEWALK RAMPS TYPE Z

STATE OF WISCONSIN
 DEPARTMENT OF TRANSPORTATION

APPROVED
 February 2022 DATE /S/ Rodney Taylor
 ROADWAY STANDARDS DEVELOPMENT ENGINEER

FHWA



INLET PROTECTION, TYPE A

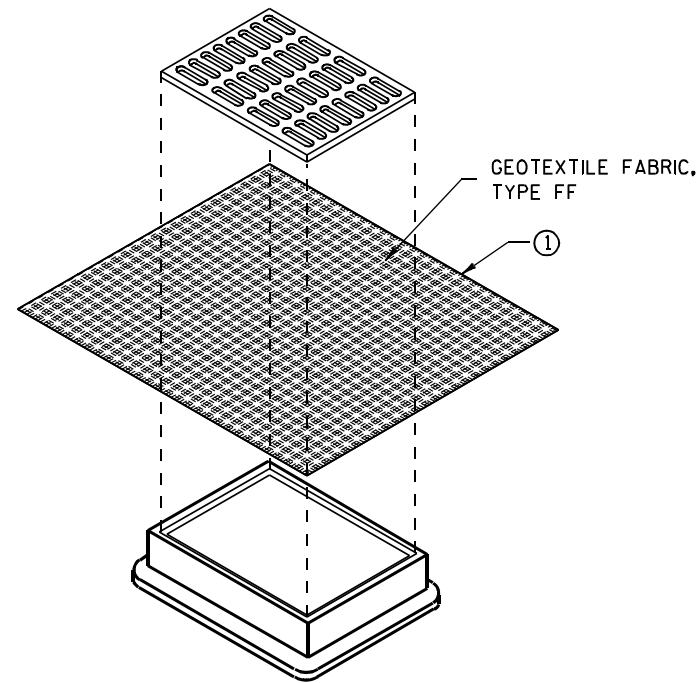
GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

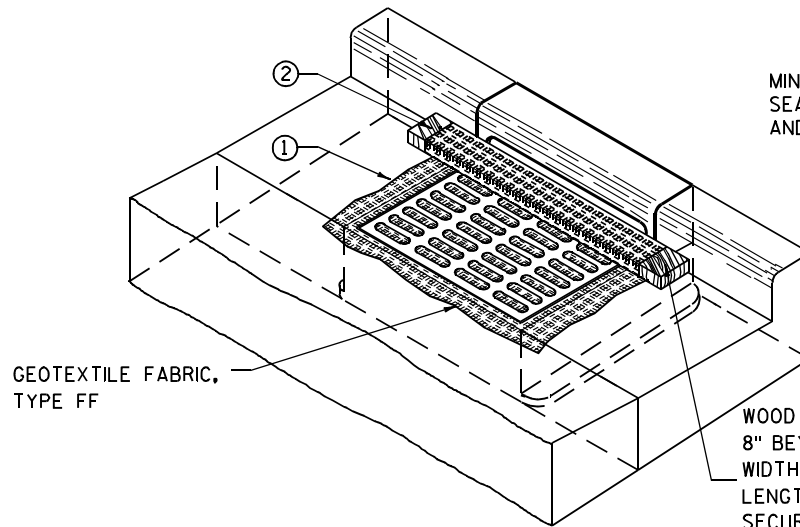
WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- ① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- ② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- ③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



**INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)**

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE C (WITH CURB BOX)

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

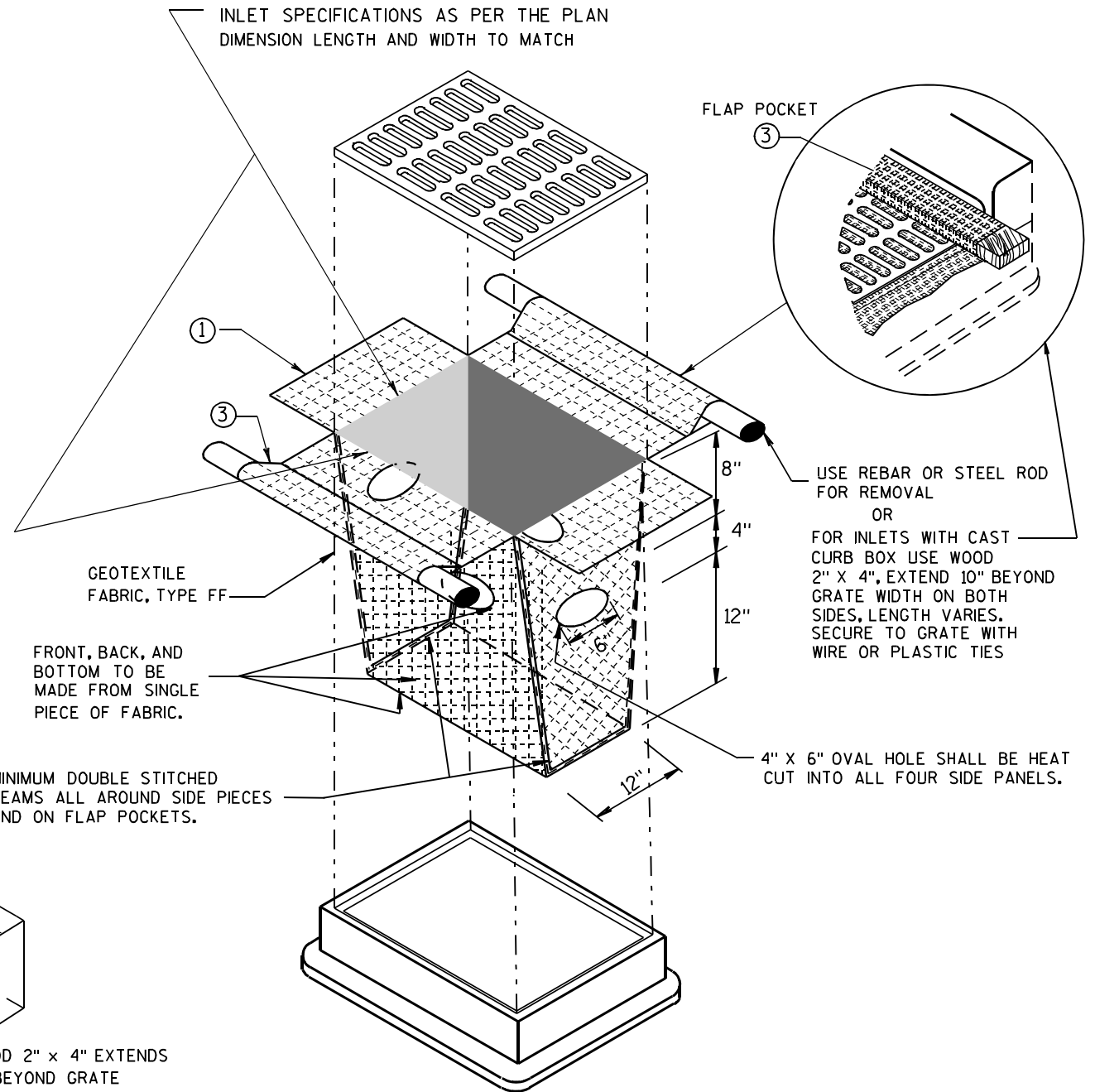
THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

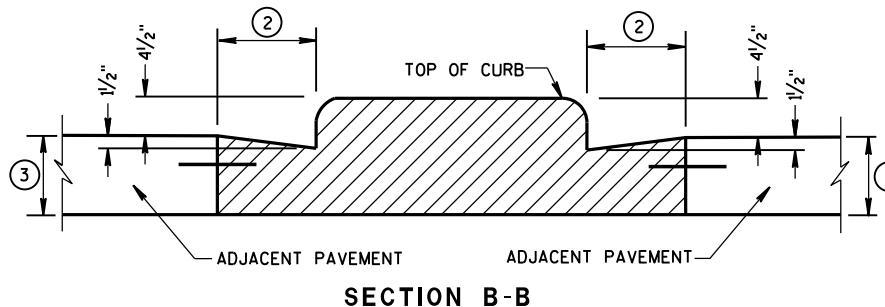
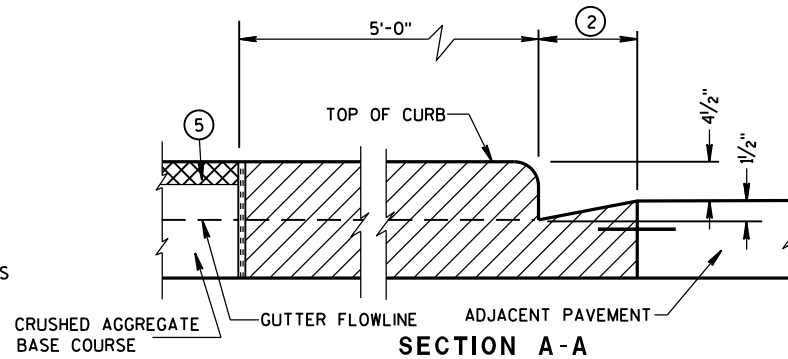
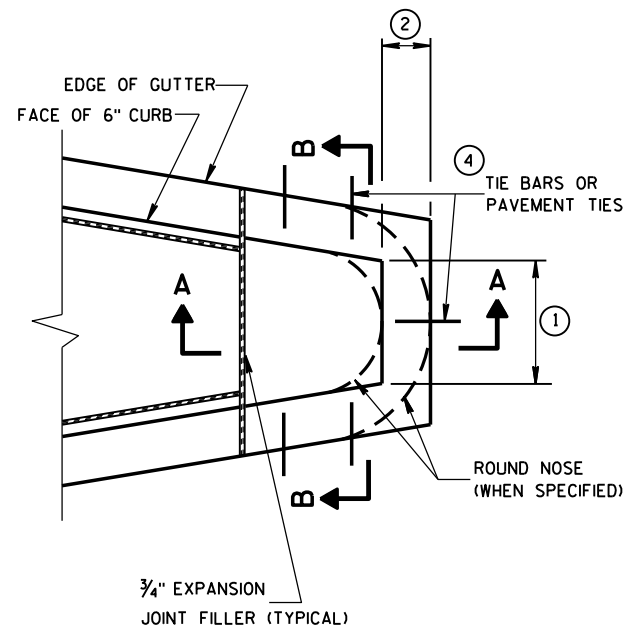
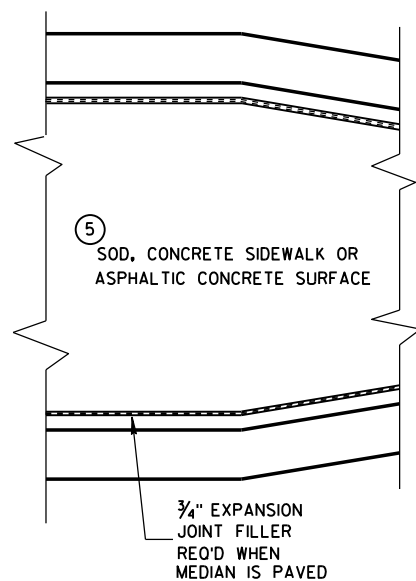
THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.



INLET PROTECTION, TYPE D

(CAN BE INSTALLED IN ANY INLET TYPE WITH OR WITHOUT A CURB BOX AS PER NOTE ②)

INLET PROTECTION TYPE A, B, C, AND D	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 10/16/02 DATE	/s/ Beth Connestra CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA	

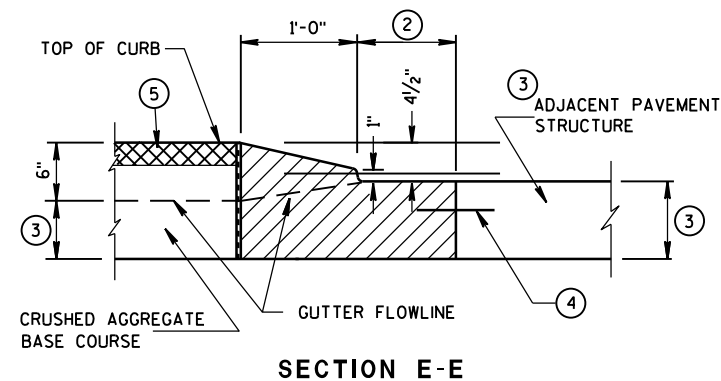
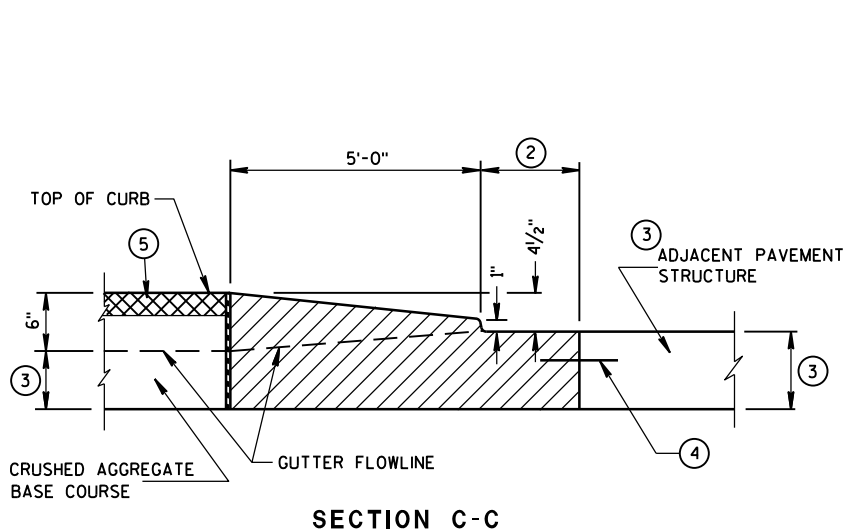
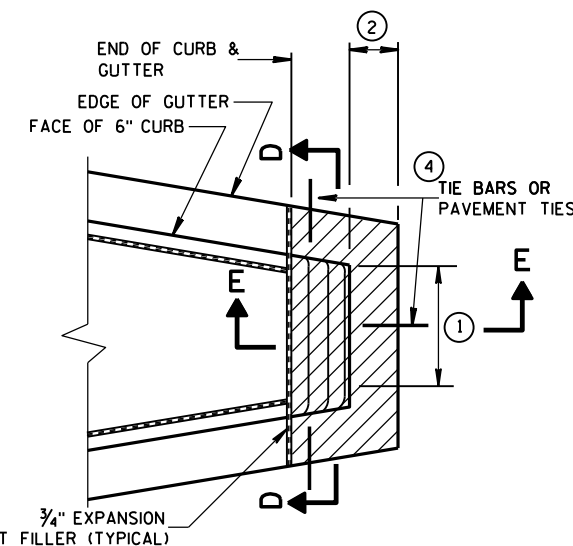


CONCRETE MEDIAN BLUNT NOSE DETAIL

GENERAL NOTES

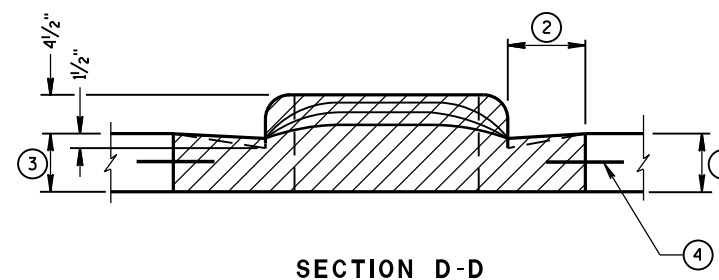
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

- ① SEE PLAN FOR MEDIAN NOSE WIDTH AND RADIUS (FOR ROUND NOSE ALTERNATE).
- ② WIDTH OF GUTTER TO MATCH EXISTING ADJACENT GUTTER OR AS SPECIFIED ELSEWHERE IN THE PLAN.
- ③ DEPTH EQUAL TO ADJACENT PAVEMENT. ADJACENT PAVEMENT STRUCTURE DETAILS ARE SHOWN ON THE PLAN. TYPICAL OPTIONS ARE:
 - (1) NEW OR EXISTING CONCRETE PAVEMENT.
 - (2) ASPHALTIC CONCRETE PAVEMENT OVER NEW OR EXISTING CONCRETE BASE COURSE.
 - (3) ASPHALTIC CONCRETE PAVEMENT OVER CRUSHED AGGREGATE BASE COURSE.
- ④ TIE BARS OR PAVEMENT TIES REQUIRED IN NEW CONCRETE PAVEMENT OR CONCRETE BASE COURSE. TIE BARS SHALL BE NO. 4 X 2'-0" SPACED AT 2'-0" C-C.
- PAVEMENT TIES REQUIRED IN EXISTING CONCRETE BASE COURSE. PAVEMENT TIES SHALL BE NO. 6 X 1'-0" SPACED AT 3'-0" C-C INSTALLED ON A HORIZONTAL SKEW OF 6:1. THE DIRECTION OF SKEW SHALL ALTERNATE AFTER EVERY ONE OR TWO BARS.
- ⑤ SURFACE TYPE AND DETAILS ARE SHOWN ELSEWHERE IN THE PLAN.



CONCRETE MEDIAN SLOPED NOSE TYPE 2

CONCRETE MEDIAN SLOPED NOSE TYPE 1



CONCRETE MEDIAN NOSE	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 6/8/2006 DATE	/s/ Jerry H. Zogg ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA	

6

6

S.D.D. 11 B 2-2

S.D.D. 11 B 2-2

GENERAL NOTES

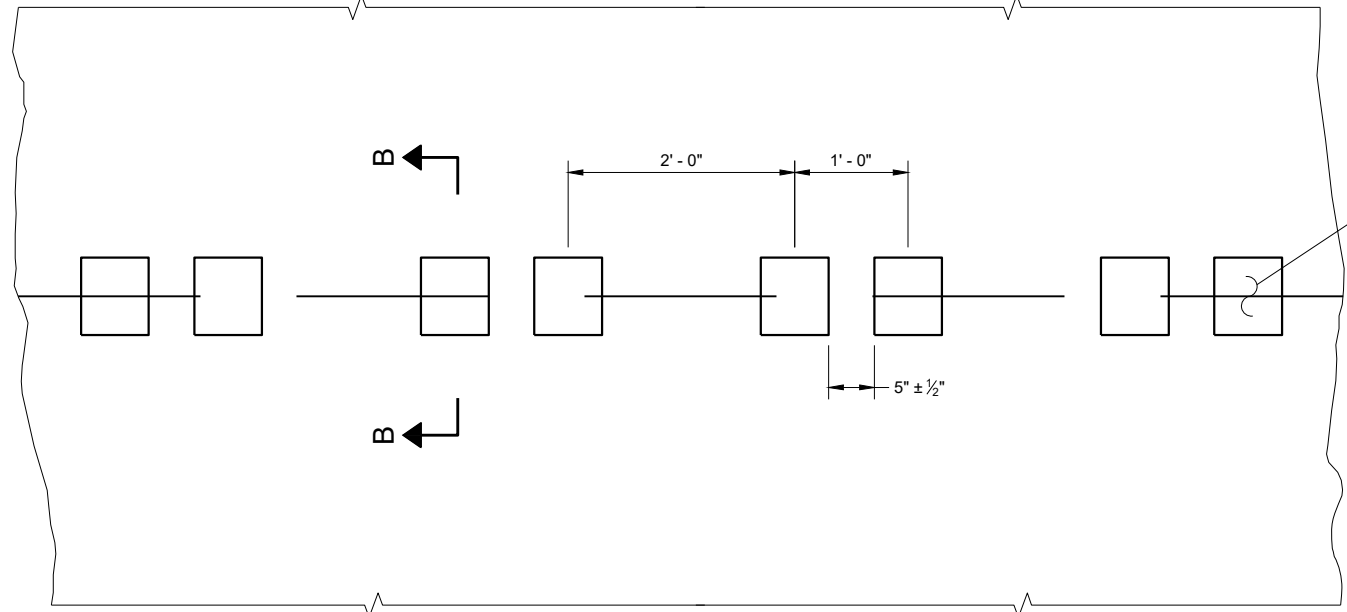
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

DO NOT MILL CENTERLINE GROOVES THROUGH ANY INTERSECTION, MARKED CROSSWALK, NON-MOTORIZED PATH CROSSING, OR SNOWMOBILE CROSSING.

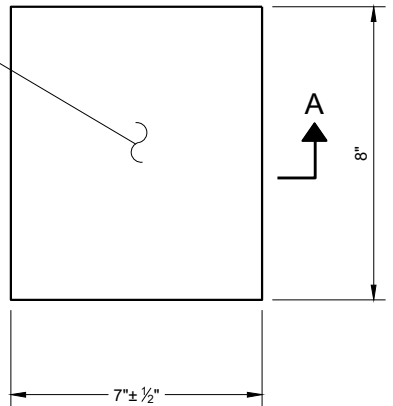
INSTALL PAVEMENT MARKING AFTER THE GROOVES ARE INSTALLED.

SEE SIGNING PLAN FOR SIGN REQUIREMENTS THAT MAY BE NEEDED.

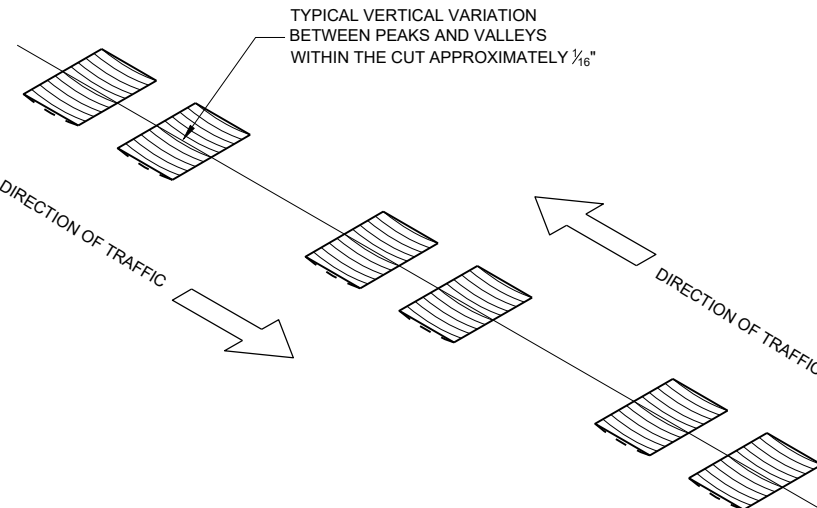
- ① CENTERLINE GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS, WHEN DIRECTED BY THE ENGINEER.



**PLAN VIEW
SHOULDER WITH GROOVES**

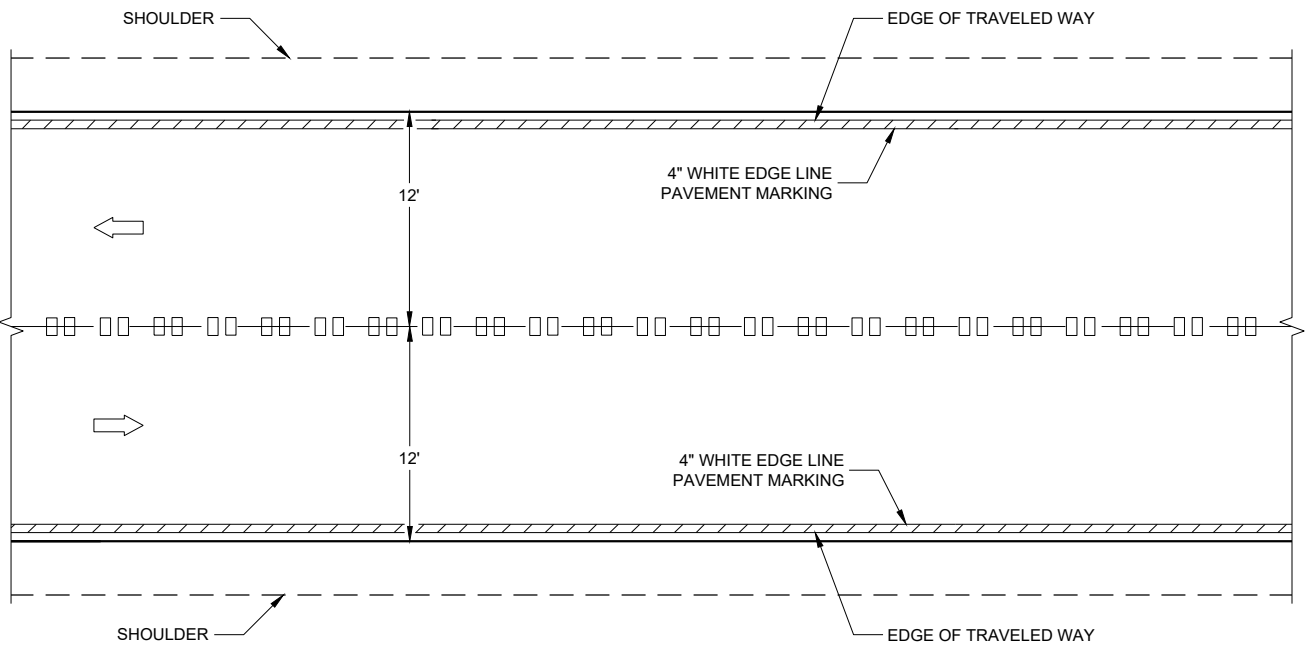


**PLAN VIEW
(SINGLE GROOVE)**

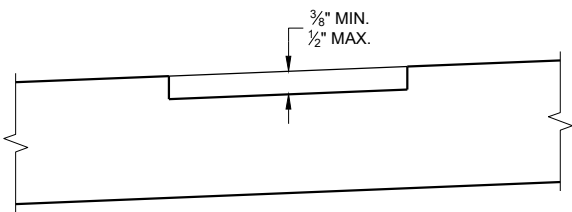


ISOMETRIC

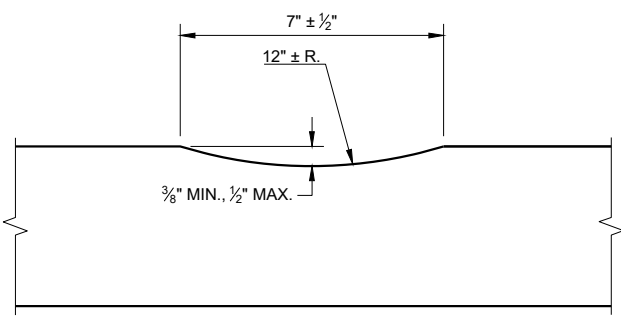
PLACEMENT DETAIL FOR TYPE 1 MILLED RUMBLE STRIP



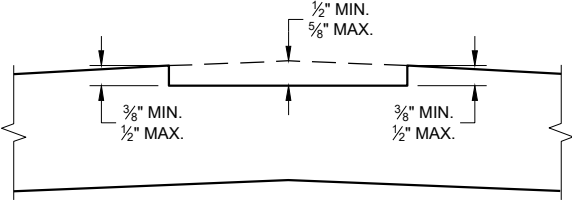
CENTERLINE GROOVES ON TWO-WAY ROADWAYS



**SECTION B - B
SUPERELEVATED ROADWAY**



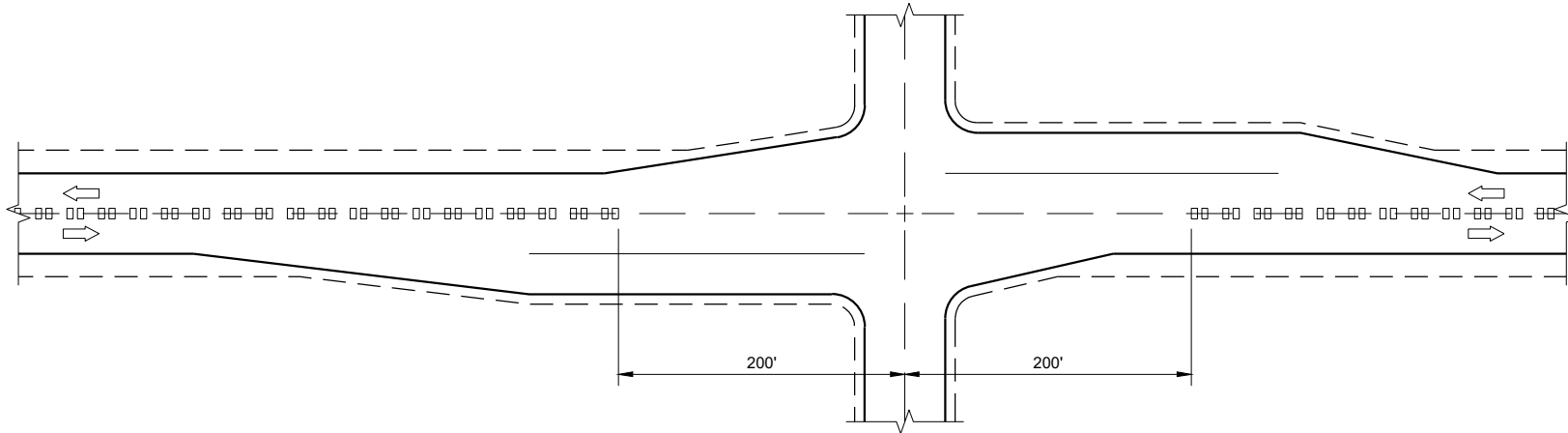
SECTION A - A



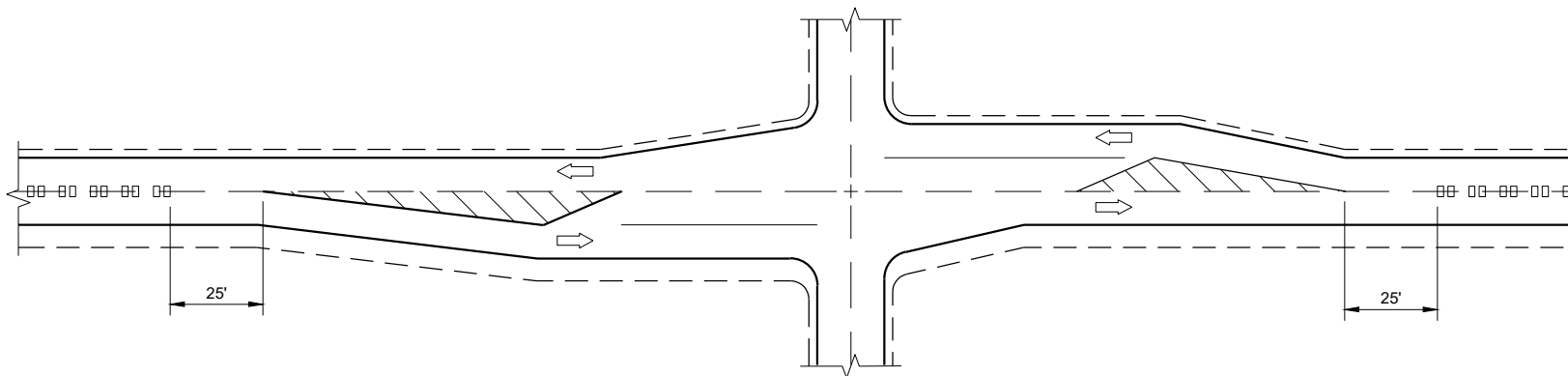
**SECTION B - B
CROWNED ROADWAY**

**2-LANE RURAL
CENTER LINE RUMBLE STRIP,
MILLING**

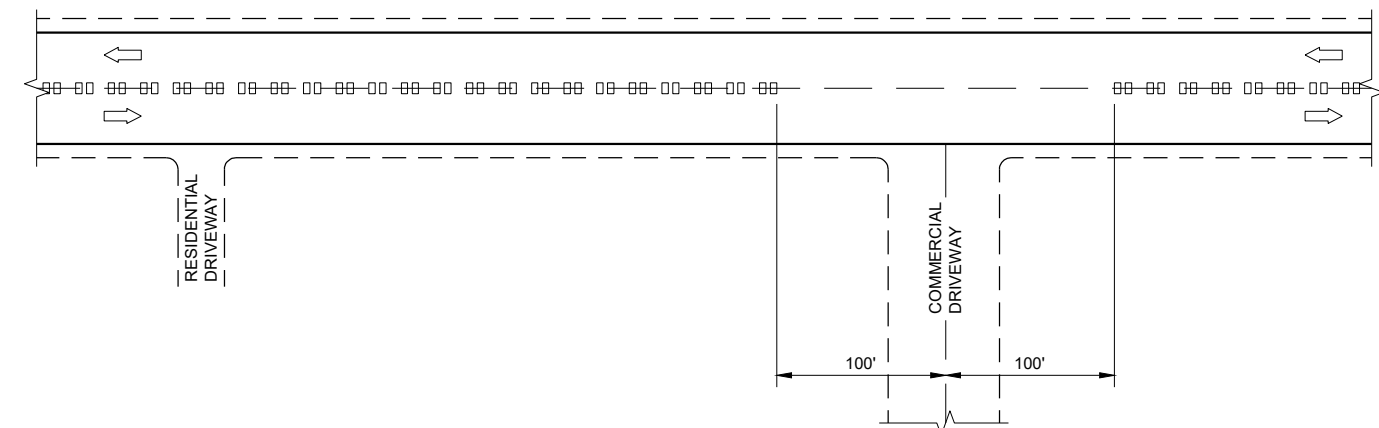
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



CENTERLINE GROOVES AT INTERSECTIONS



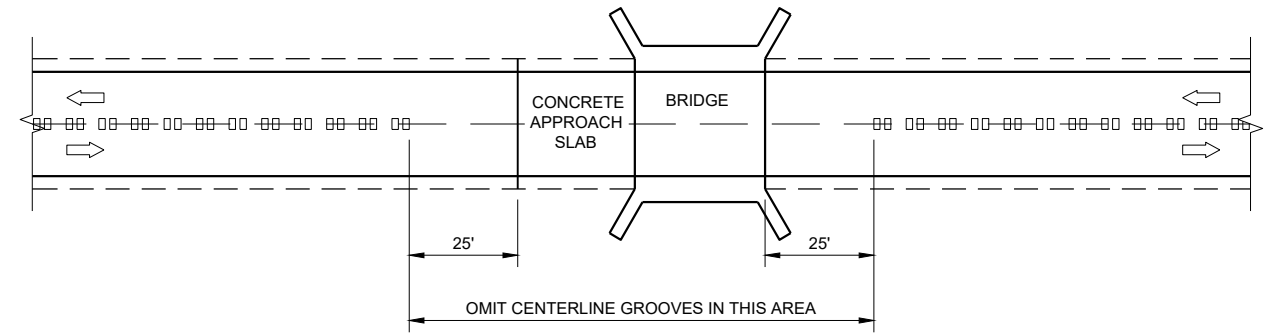
**CENTERLINE GROOVES AT INTERSECTIONS
(WITH LEFT TURN LANES)**



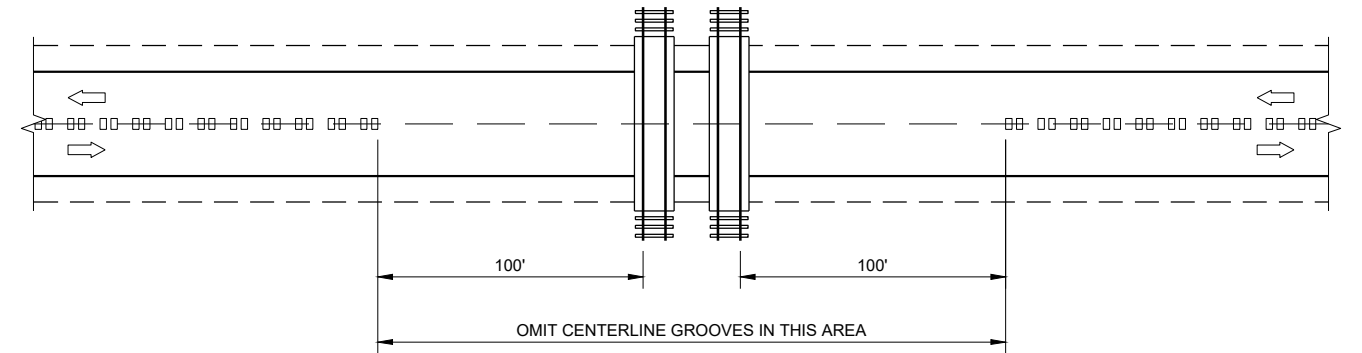
CENTERLINE GROOVES AT DRIVEWAYS^①

GENERAL NOTES

- ① CENTERLINE GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS. WHEN DIRECTED BY THE ENGINEER.



CENTERLINE GROOVES AT BRIDGES



CENTERLINE GROOVES AT RAILROADS

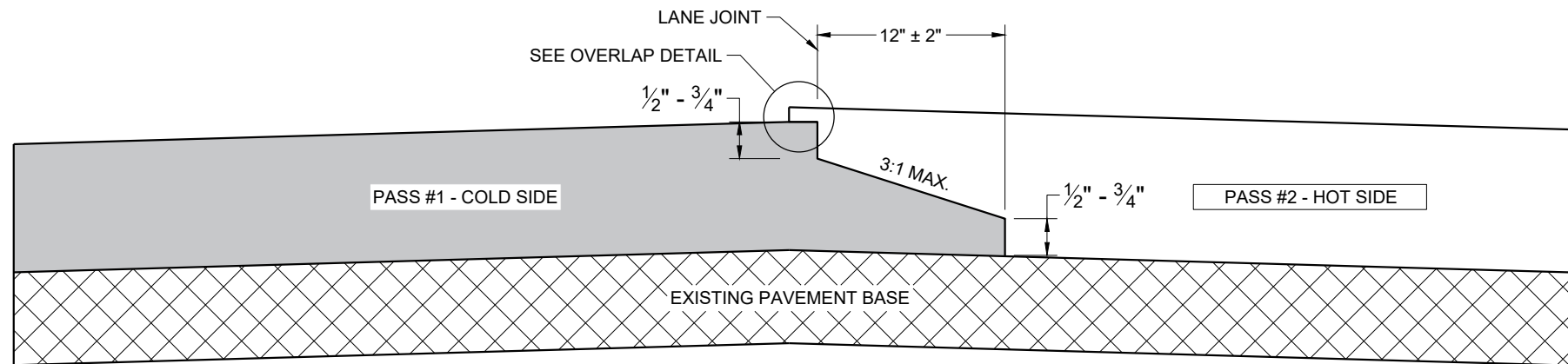
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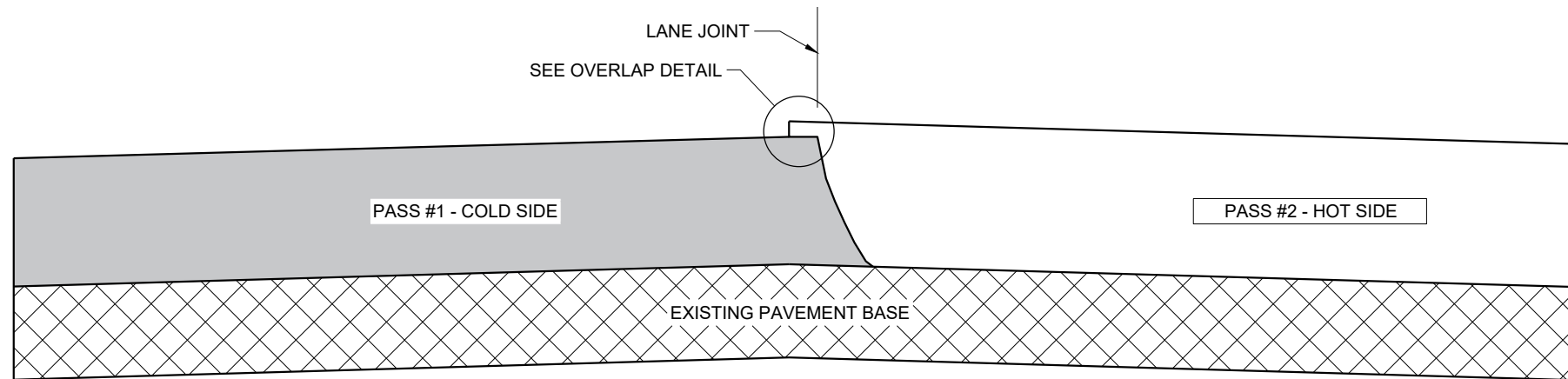
SDD 13A11 - 03b

SDD 13A11 - 03b

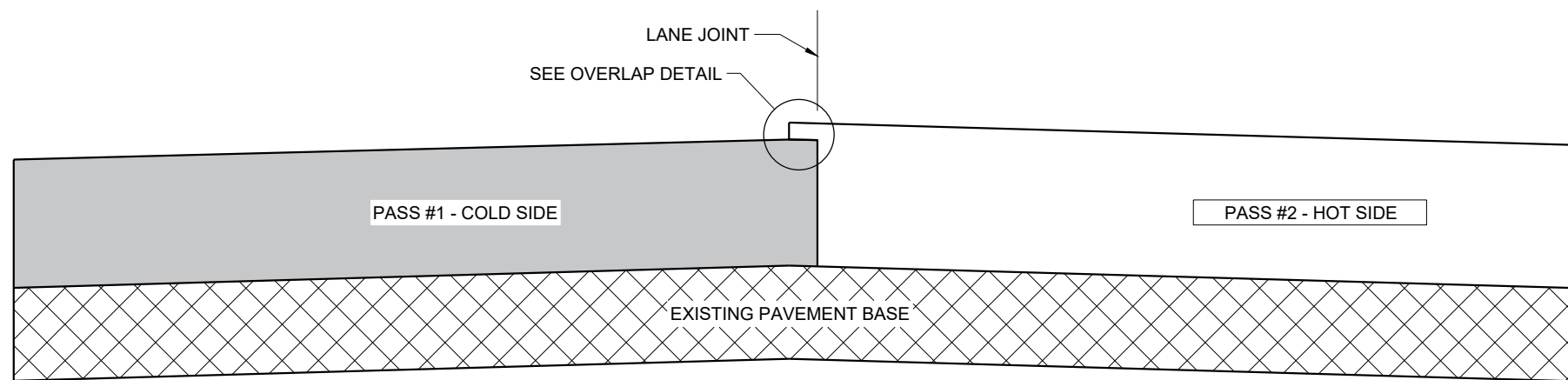
2-LANE RURAL CENTERLINE RUMBLE STRIP, MILLING	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 7/2018 DATE	/S/ Rodney Taylor ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA	



**TYPICAL PAVEMENT CROSS SECTION
NOTCHED WEDGE JOINT**



**TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT**



**TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT (MILLED)**

GENERAL NOTES

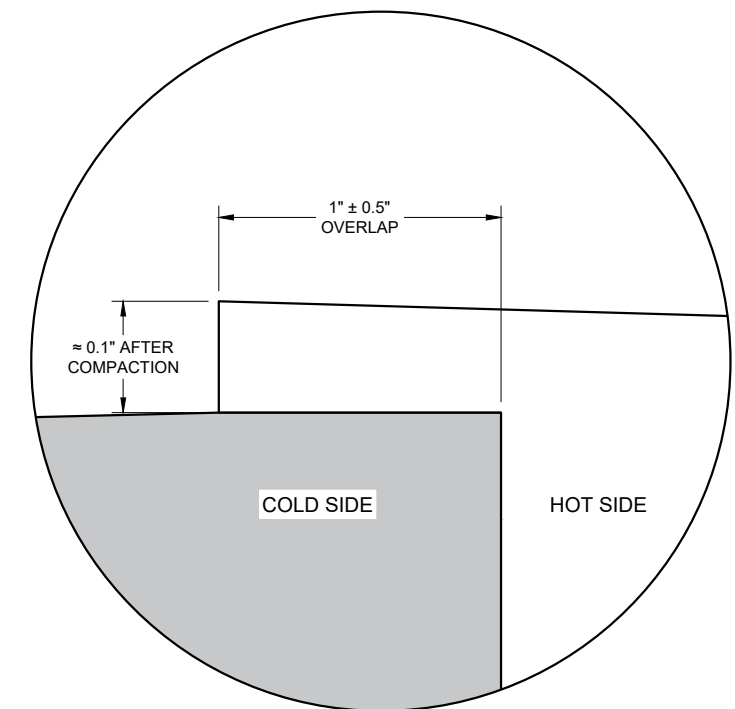
IN ADDITION TO THE DETAILS PROVIDED IN THIS DRAWING, CONFORM TO STANDARD SPECIFICATION 450.3.2.8 FOR WHEN A NOTCHED WEDGE JOINT IS REQUIRED AND FOR GENERAL JOINT CONSTRUCTION REQUIREMENTS.

FOR ALL LONGITUDINAL JOINTS, ENSURE THE PAVER SCREED OVERLAPS THE PREVIOUSLY PLACED PAVEMENT BY $1" \pm 0.5"$ AND THE HOT SIDE OF THE JOINT REMAINS HIGHER THAN THE COLD SIDE BY APPROXIMATELY 0.1" AFTER FINAL COMPACTION. (IT WILL BE FLUSH WHEN PAVING IN ECHELON.)

ONLY REMOVE THE LONGITUDINAL NOTCHED WEDGE JOINT FOR SMA PAVEMENT OR AS DIRECTED BY THE ENGINEER TO ADDRESS SPECIFIC LENGTHS OF JOINT DAMAGED BY TRAFFIC.

WHEN MILLING BACK OR REMOVING ANY LONGITUDINAL JOINT, LIMIT THE MATERIAL REMOVED TO 2" FROM THE TOP NOTCH OR FROM THE VERTICAL JOINT EDGE ON THE COLD SIDE OF THE JOINT.

USE LONGITUDINAL MILLED JOINT AS PLANS SHOW OR THE AS THE ENGINEER DIRECTS.



OVERLAP DETAIL (TYPICAL)

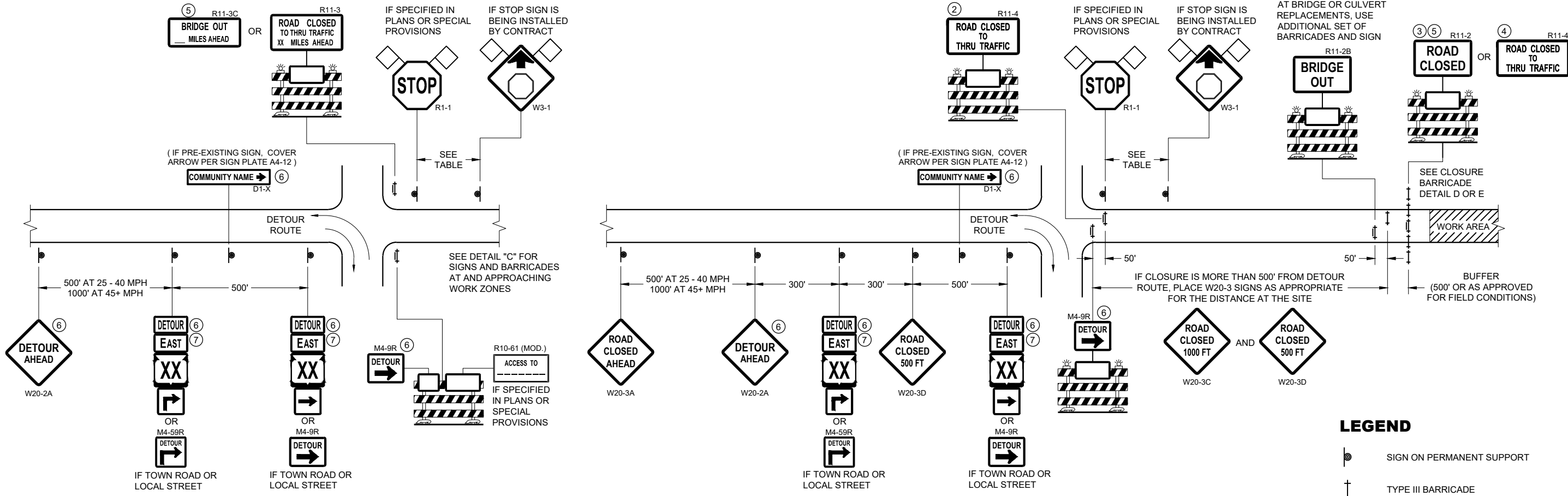
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SDD 13C19 - 03

SDD 13C19 - 03

HMA LONGITUDINAL JOINTS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED November 2020 DATE	/S/ Steven Hefel HMA PAVEMENT ENGINEER
FHWA	



**DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR**

WORK ZONE GREATER THAN OR EQUAL TO 1/2 MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)

**DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR**

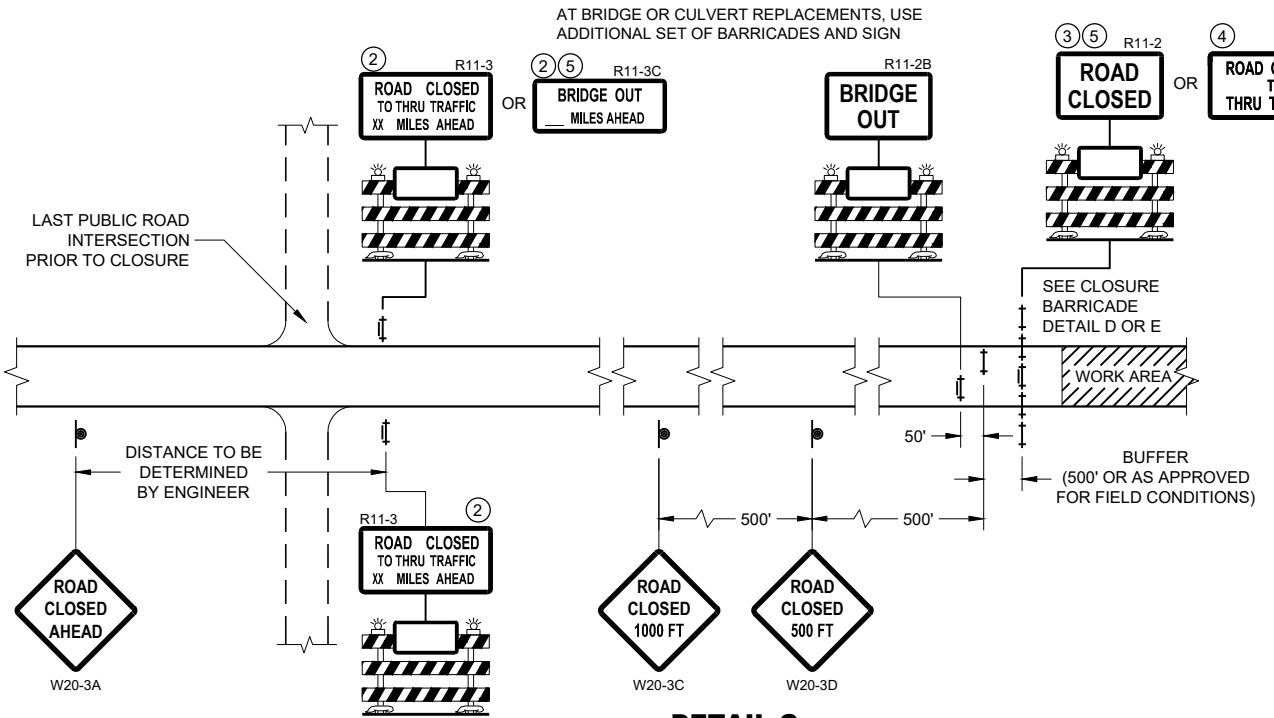
WORK ZONE LESS THAN 1/2 MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
- FLAGS, 16" X 16" MIN. (ORANGE)

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

- M4 - 8
- M3 - X
- M1 - 4
- M1 - 6
- M1 - 5A
- M05 - 1
- M06 - 1



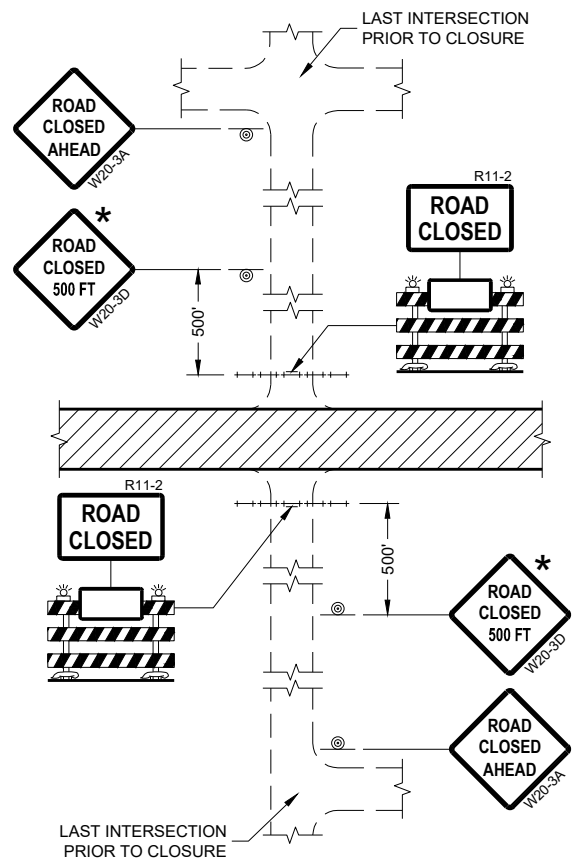
**DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR**

SEE SDD 15C2-SHEET "b"
FOR GENERAL NOTES
AND FOOTNOTES ① THROUGH ⑦

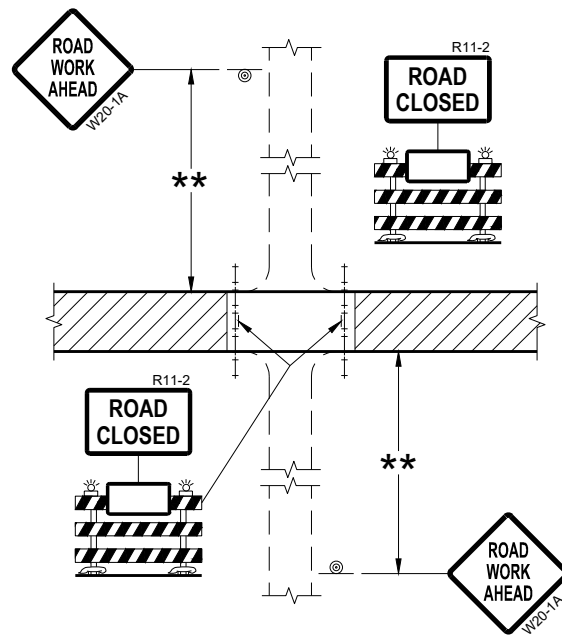
**BARRICADES AND SIGNS
FOR MAINLINE CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

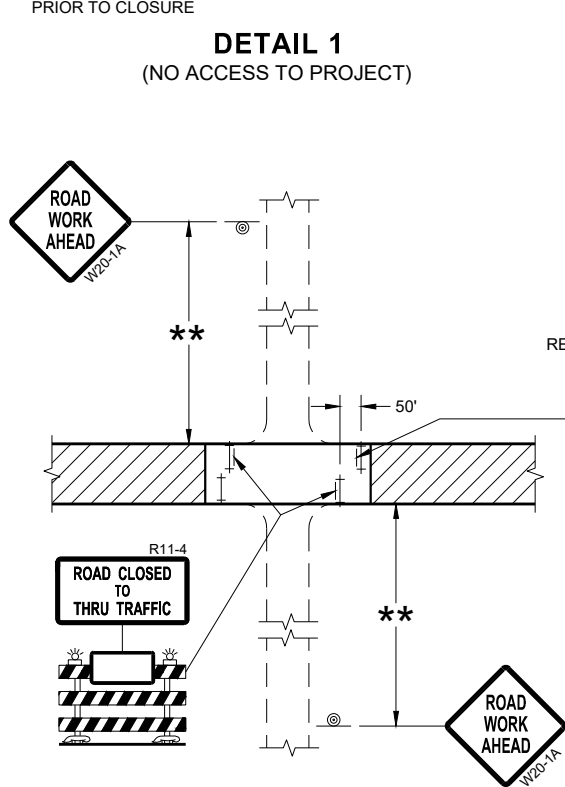
APPROVED
February 2020 /S/ Andrew Heidtke
DATE DATE WORK ZONE ENGINEER
FHWA



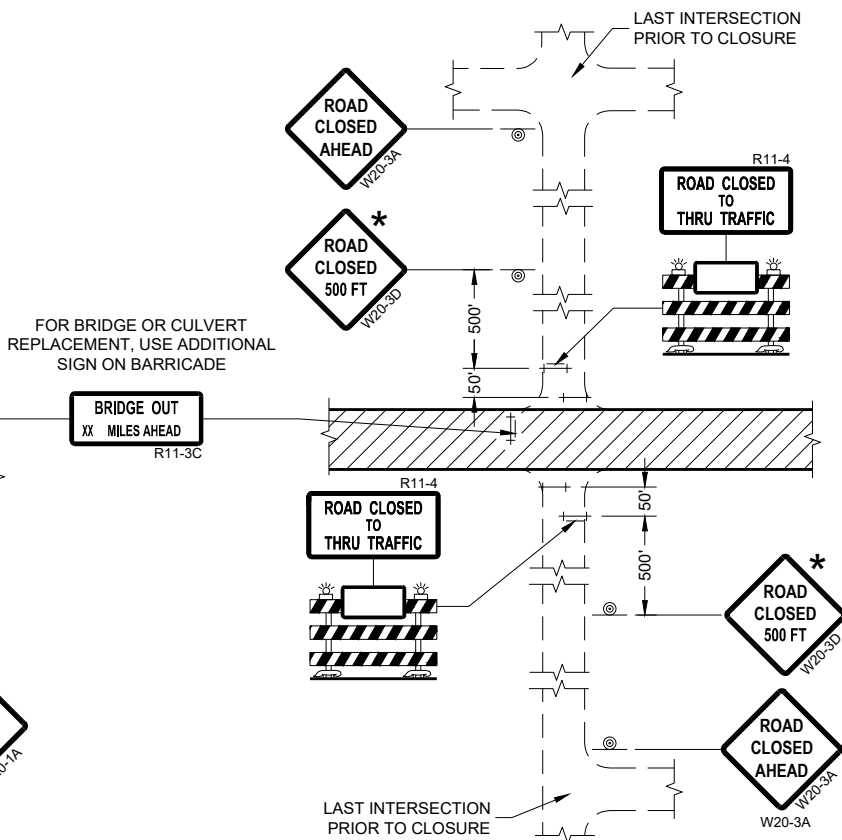
DETAIL 1
(NO ACCESS TO PROJECT)



DETAIL 2
(PUBLIC CROSS-TRAFFIC MAINTAINED.
NO ACCESS TO PROJECT)



DETAIL 3
(PUBLIC CROSS-TRAFFIC MAINTAINED.
CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)



DETAIL 4
(CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE (500 FEET DESIRABLE) TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3, AND R11-4 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
R11-2 SHALL BE 48" X 30".
R11-4 AND R11-3 SHALL BE 60" X 30".

- * OMIT THE "ROAD CLOSED 500 FT." SIGN IF THE LAST INTERSECTION IS 500 FEET OR LESS FROM THE WORK ZONE.
- ** 500' MAX. OR AT LAST INTERSECTION, WHICHEVER IS CLOSEST.

LEGEND

- ⊙ SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- ⚡ TYPE "A" WARNING LIGHT (FLASHING)
- ▨ WORK AREA

**BARRICADES AND SIGNS
FOR
SIDEROAD CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
July 2018 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.




ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

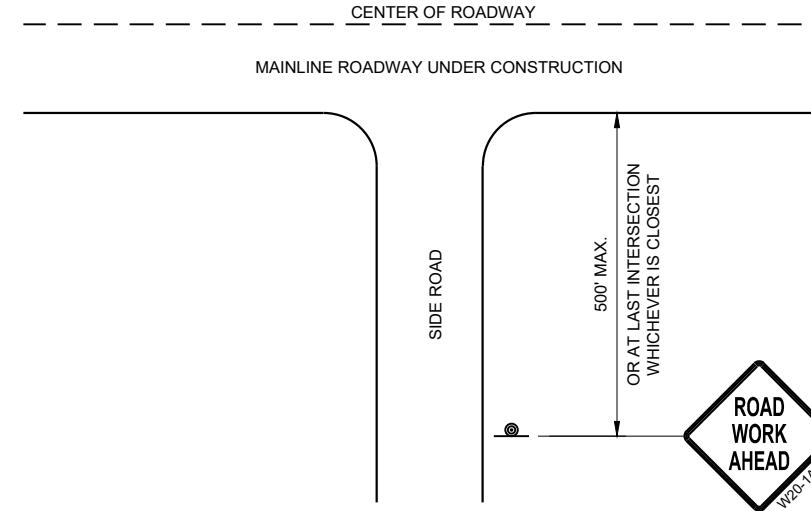
SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

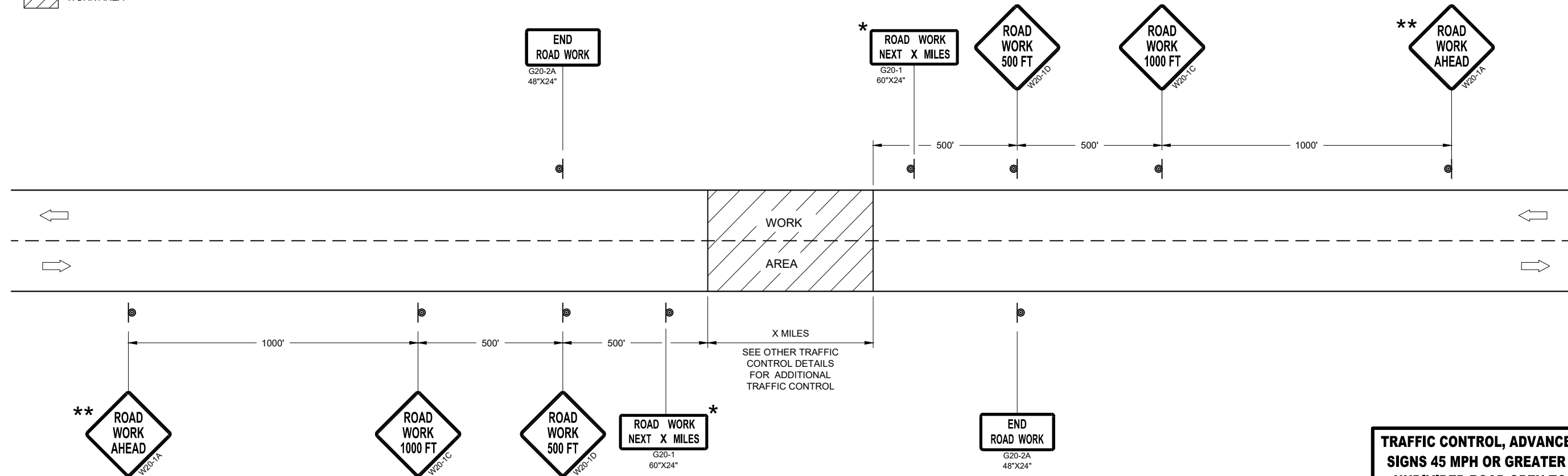
- * OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS
- ** PLACE AN ADDITIONAL W20-1A "ROAD WORK AHEAD" SIGN IF WORK AREA WITHIN THE PROJECT IS SEPARATED BY MORE THAN 2 MILES FROM PREVIOUS WORK AREA.

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC
-  WORK AREA



**TYPICAL SIDE ROAD APPROACH
WARNING SIGN DETAIL**



TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45MPH OR GREATER

**TRAFFIC CONTROL, ADVANCE WARNING
SIGNS 45 MPH OR GREATER TWO-WAY
UNDIVIDED ROAD OPEN TO TRAFFIC**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE July 2018 /S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.




ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS, 36"X36" SIGNS MAY BE USED INSTEAD OF 48" X 48" SIGNS.

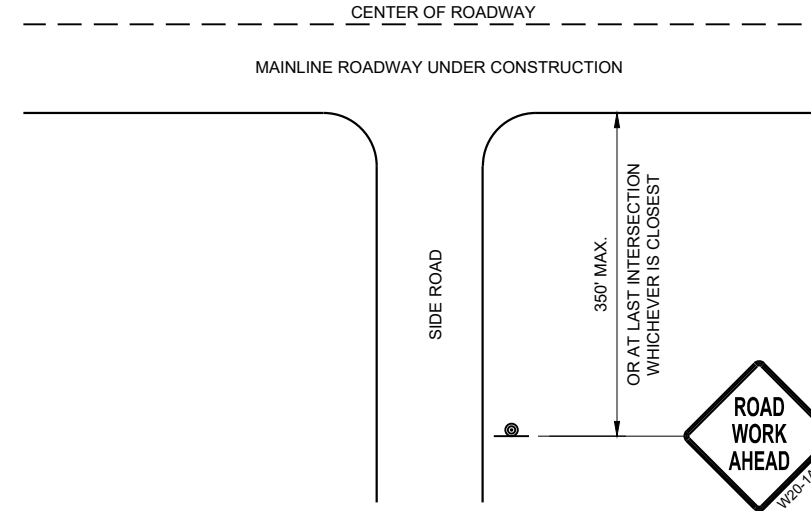
SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

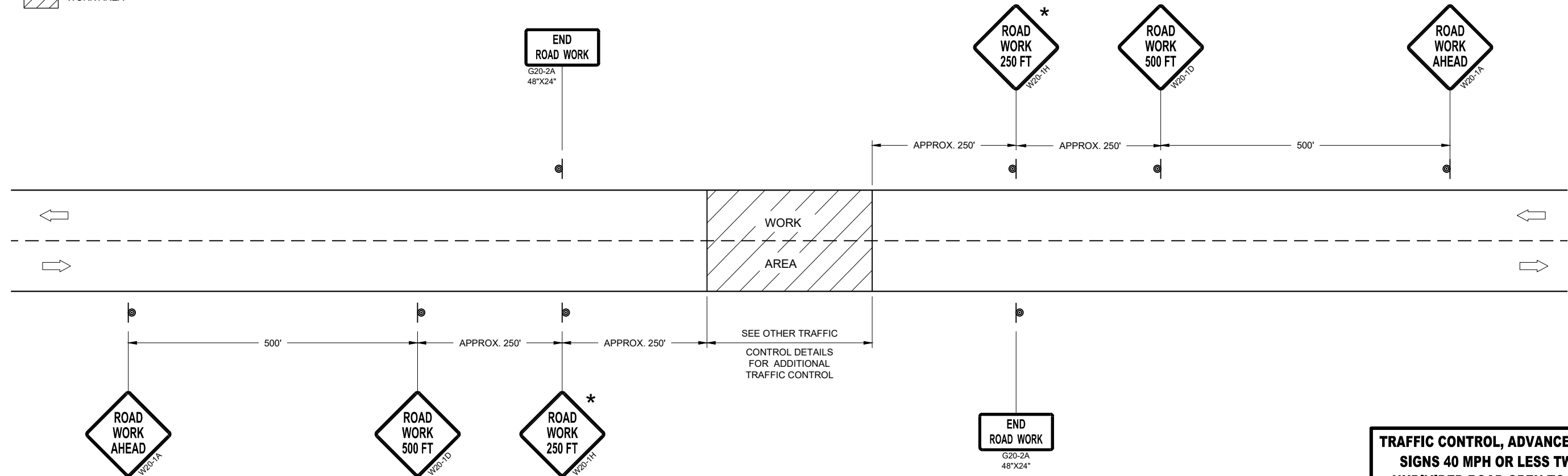
* THE THIRD W20-1 SIGN IS REQUIRED ONLY IF THERE IS AN INTERSECTION BETWEEN THE "ROAD WORK 500 FEET" SIGN AND THE WORK ZONE. ADJUST THE PLACEMENT OF THIS SIGN BASED ON INTERSECTION LOCATION AND OTHER FIELD CONDITIONS.

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC
-  WORK AREA



**TYPICAL SIDE ROAD APPROACH
WARNING SIGN DETAIL**



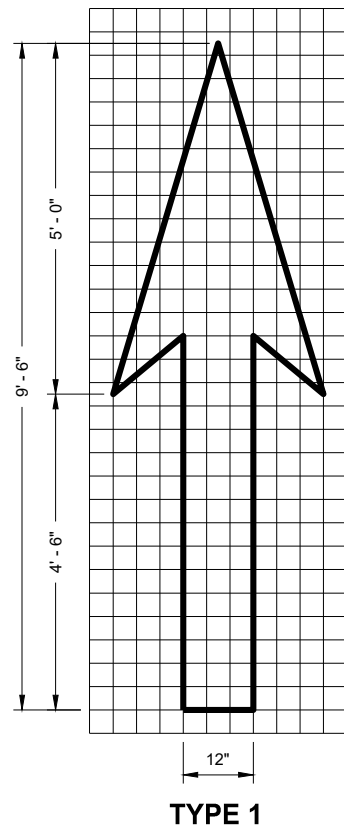
TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40MPH OR LESS

**TRAFFIC CONTROL, ADVANCE WARNING
SIGNS 40 MPH OR LESS TWO-WAY
UNDIVIDED ROAD OPEN TO TRAFFIC**

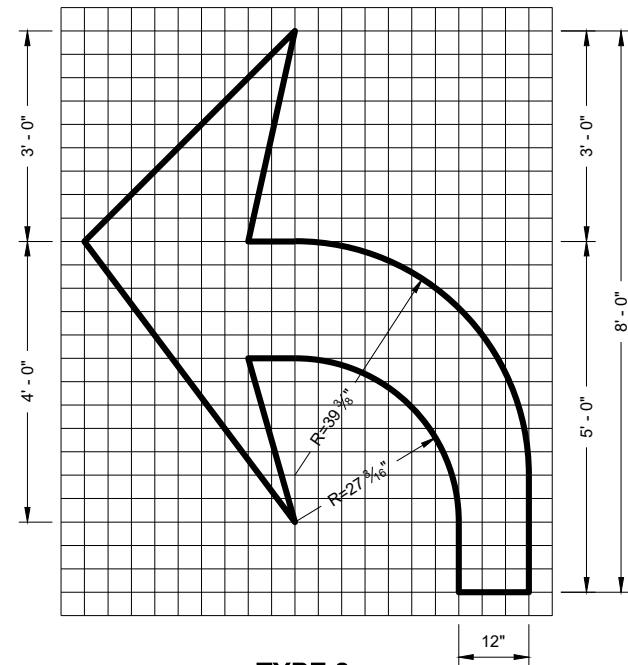
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
July 2018 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

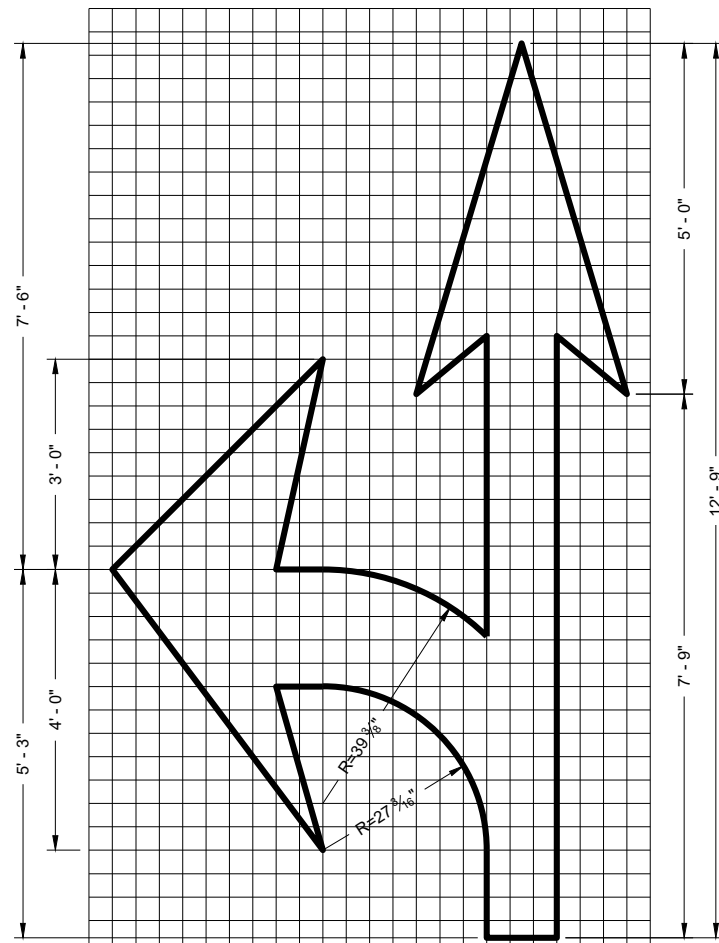
FHWA



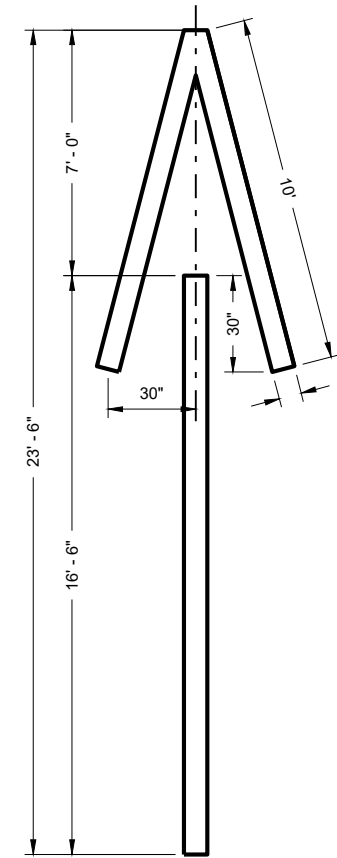
TYPE 1



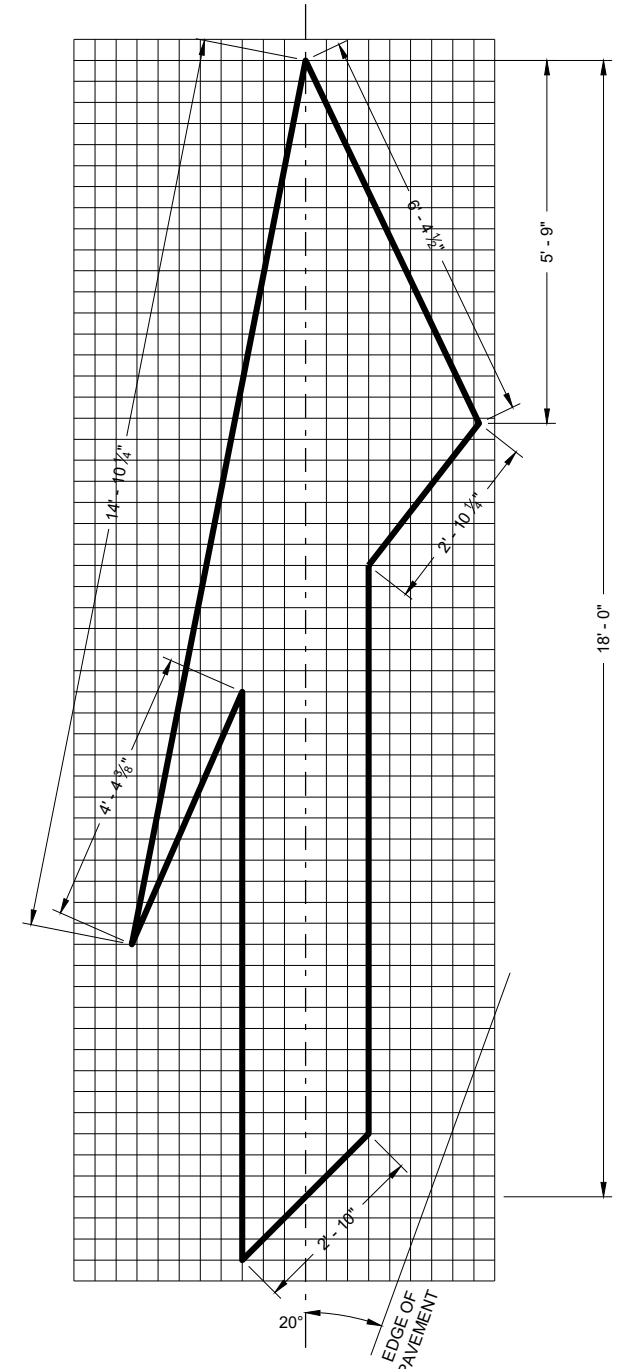
TYPE 2



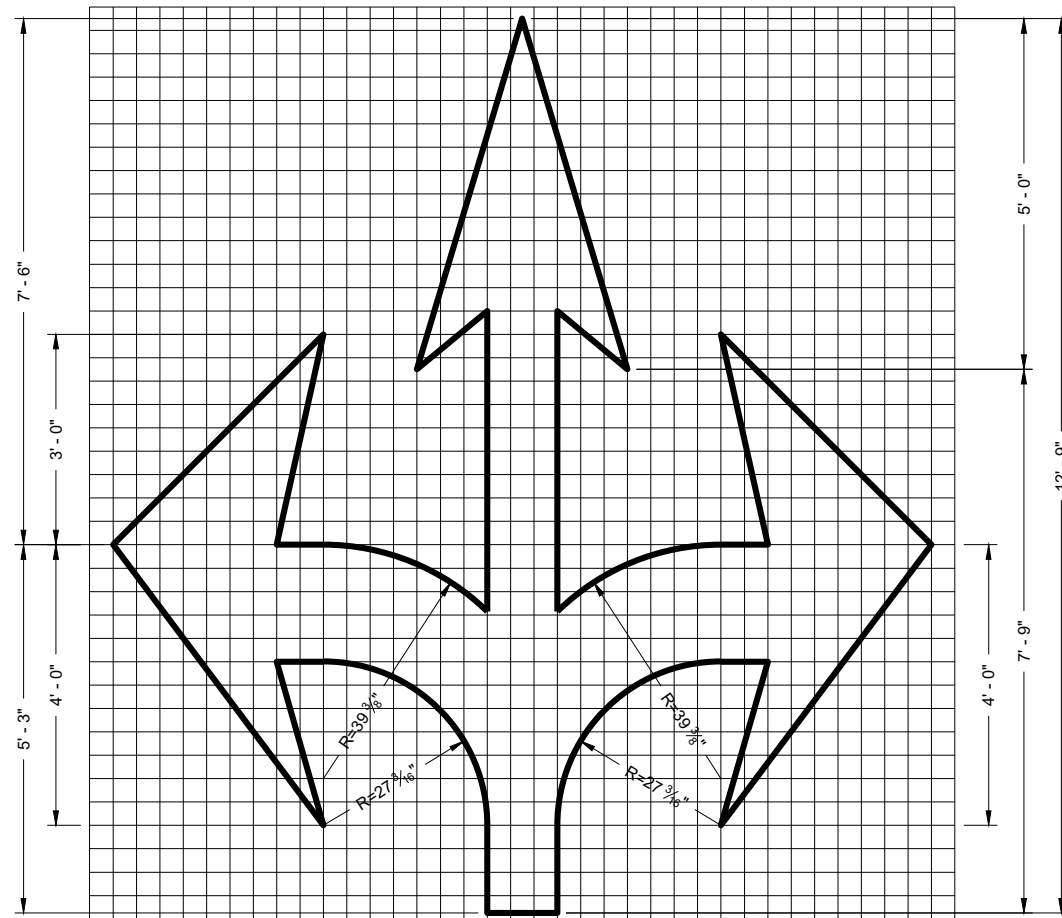
TYPE 3



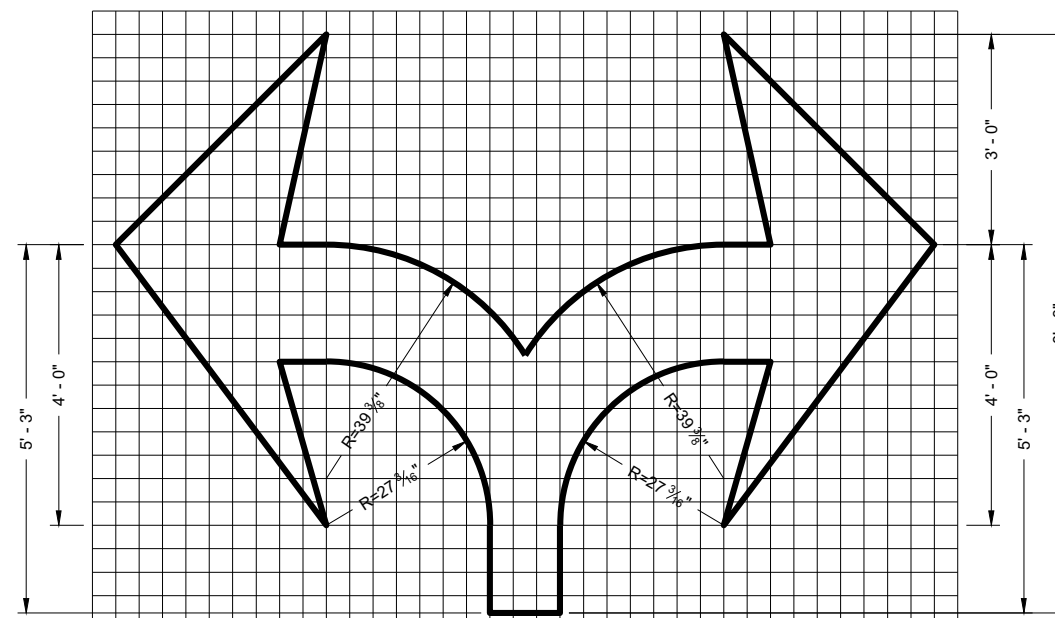
TYPE 4



TYPE 5 LANE DROP ARROW



TYPE 6



TYPE 7

GENERAL NOTES

DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

PAVEMENT MARKING ARROWS

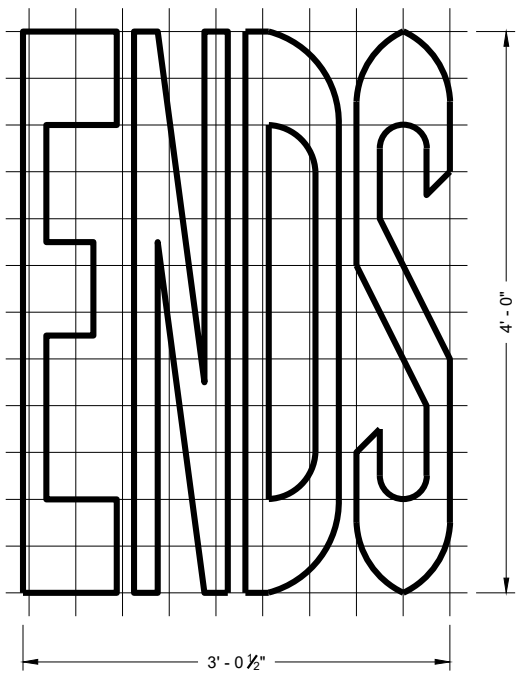
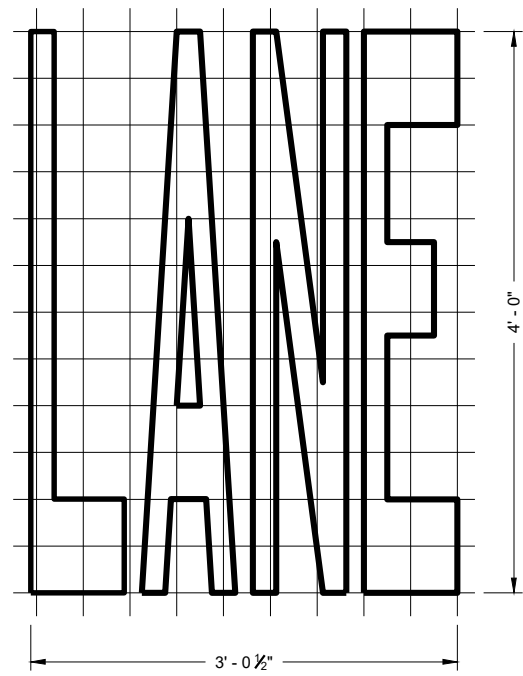
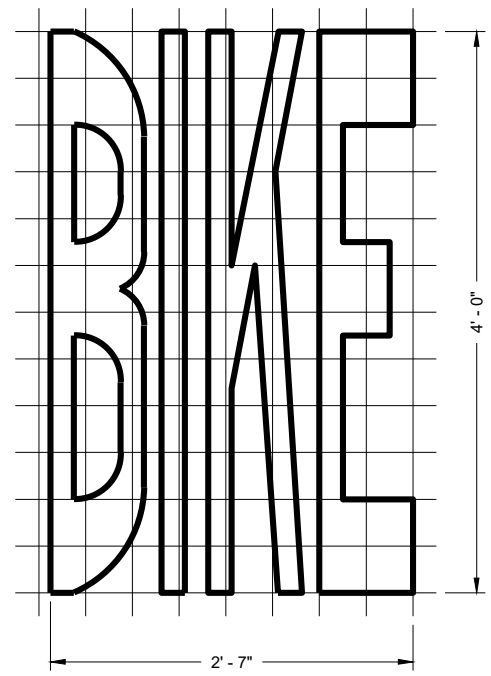
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2019
DATE /S/ Matthew Rauch
STATE SIGNING AND MARKING
ENGINEER

FHWA



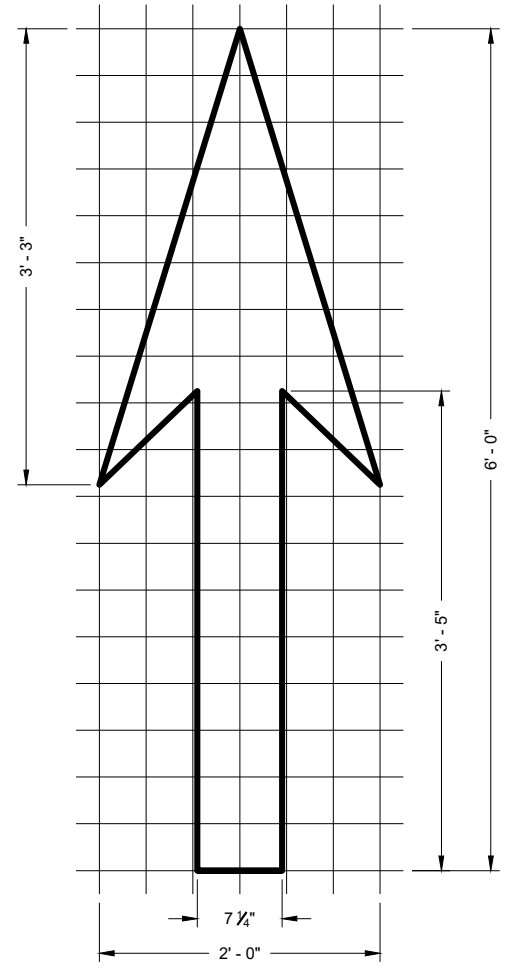
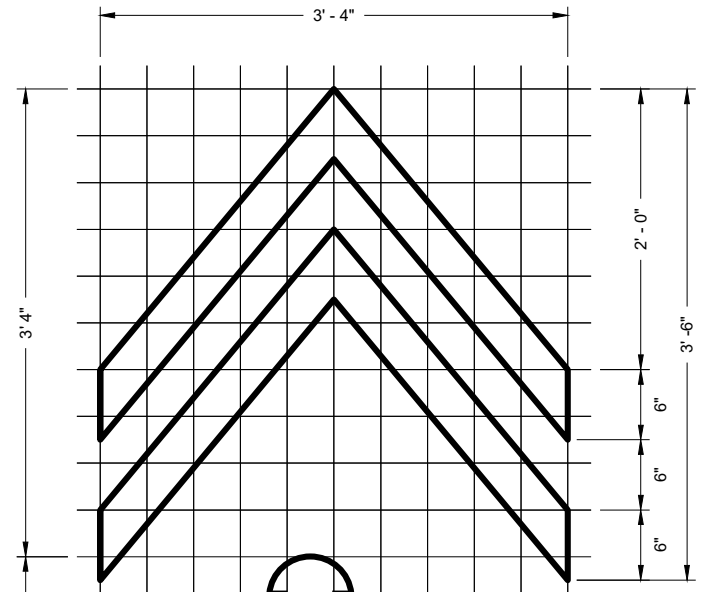
SDD 15C7-e Pavement Marking For Bike Lanes



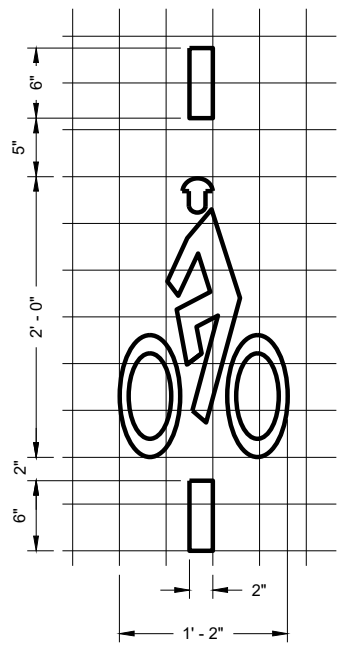
BIKE LANE WORDS

GENERAL NOTES

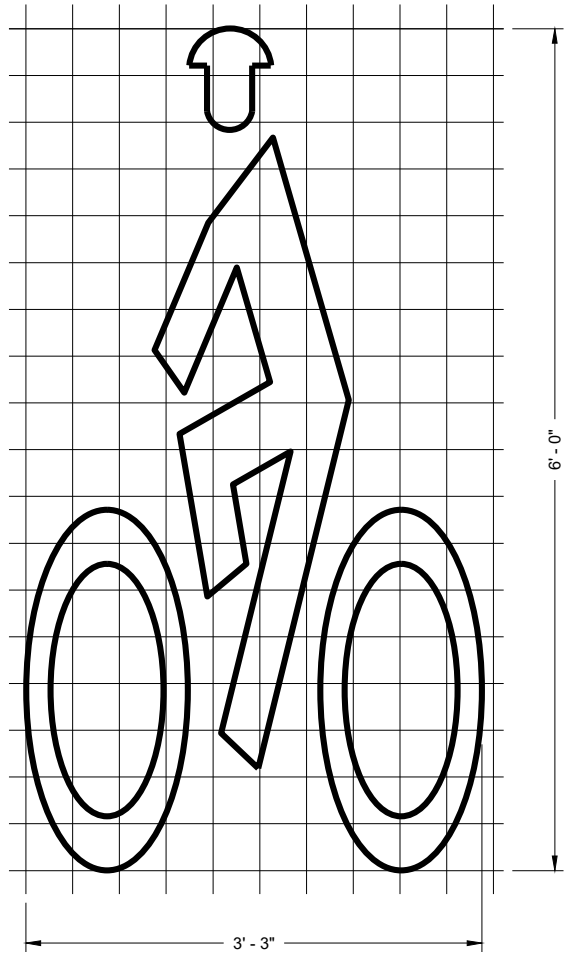
DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.



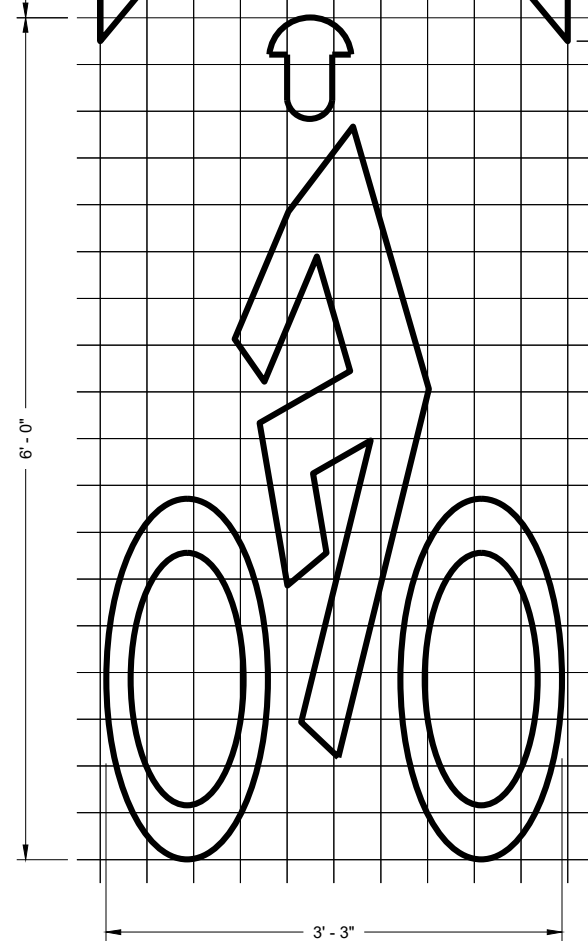
BIKE LANE ARROW



BICYCLE DETECTOR PAVEMENT MARKING



BIKE LANE SYMBOL



BIKE LANE SYMBOL FOR SHARED LANE

6

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SDD 15C07 - 15e

SDD 15C07 - 15e




PAVEMENT MARKING FOR BIKE LANES	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED November 2019 DATE	/s/ Matthew Rauch STATE SIGNING AND MARKING ENGINEER
FHWA	

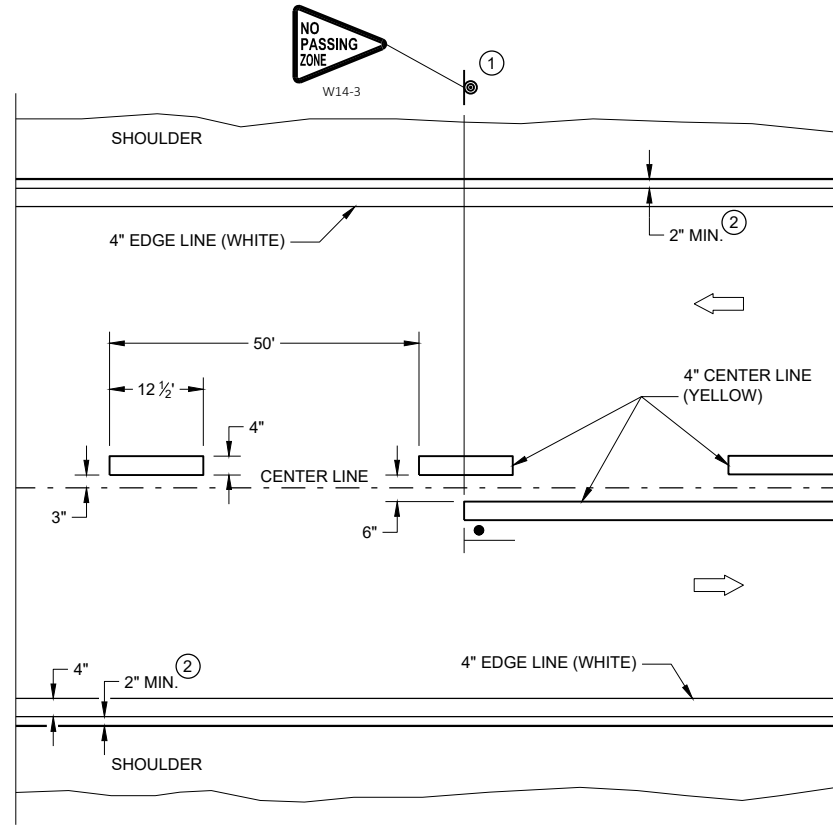
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

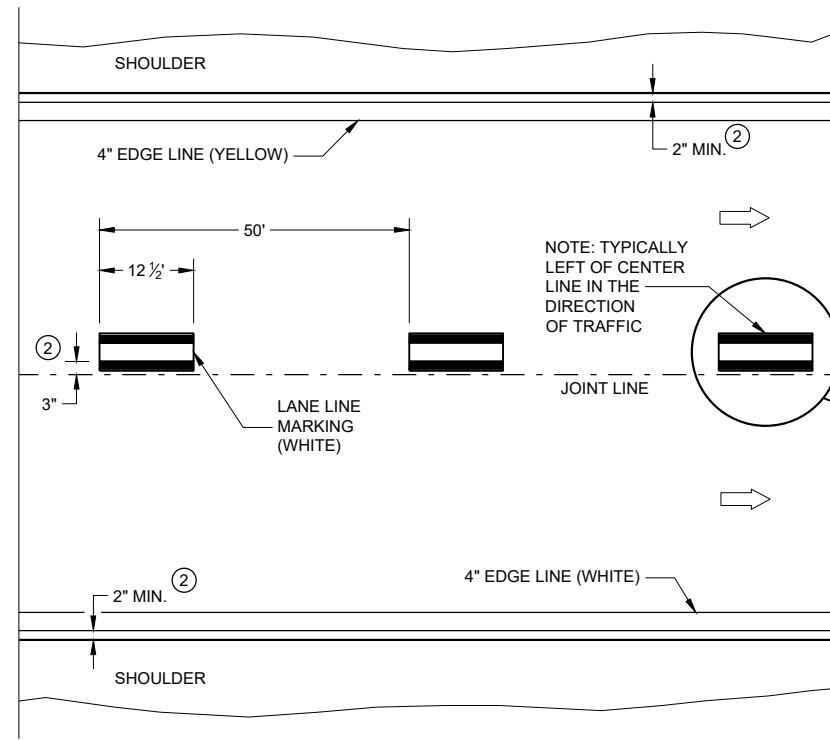
- ① LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING
- ② MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

LEGEND

-  "T" MARKING
-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC

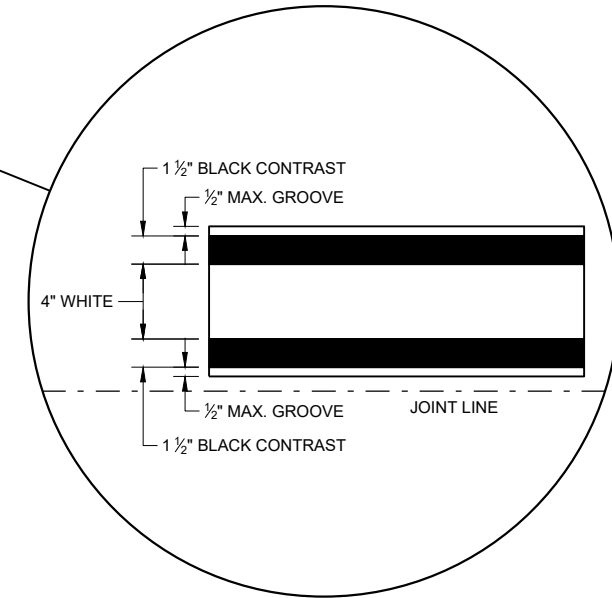


TWO WAY TRAFFIC



ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING



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SDD 15C08 - 21a

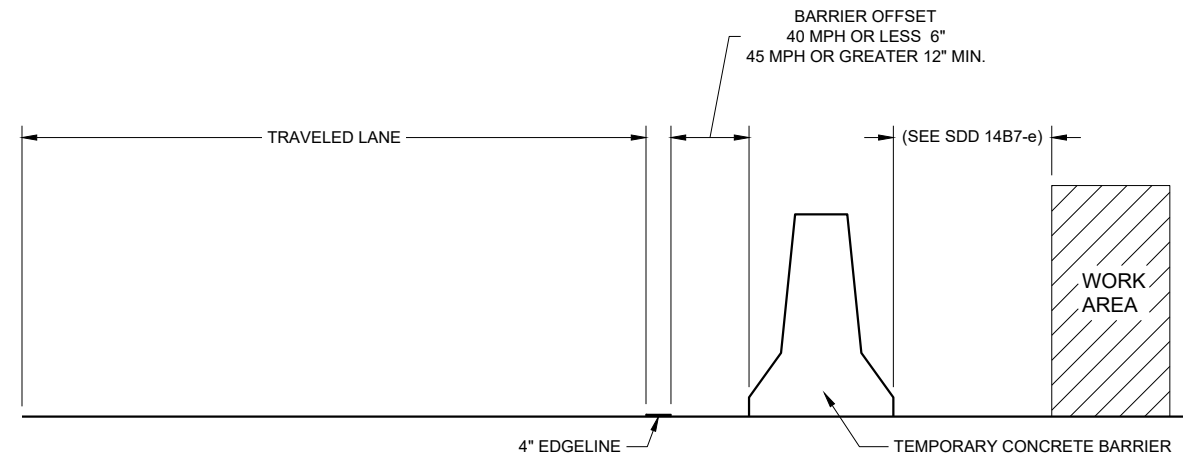
SDD 15C08 - 21a

PERMANENT LONGITUDINAL PAVEMENT MARKINGS

STATE OF WISCONSIN
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APPROVED
DATE: May 2022 /S/ Jeannie Silver
STATEWIDE SIGNING AND MARKING ENGINEER

FHWA



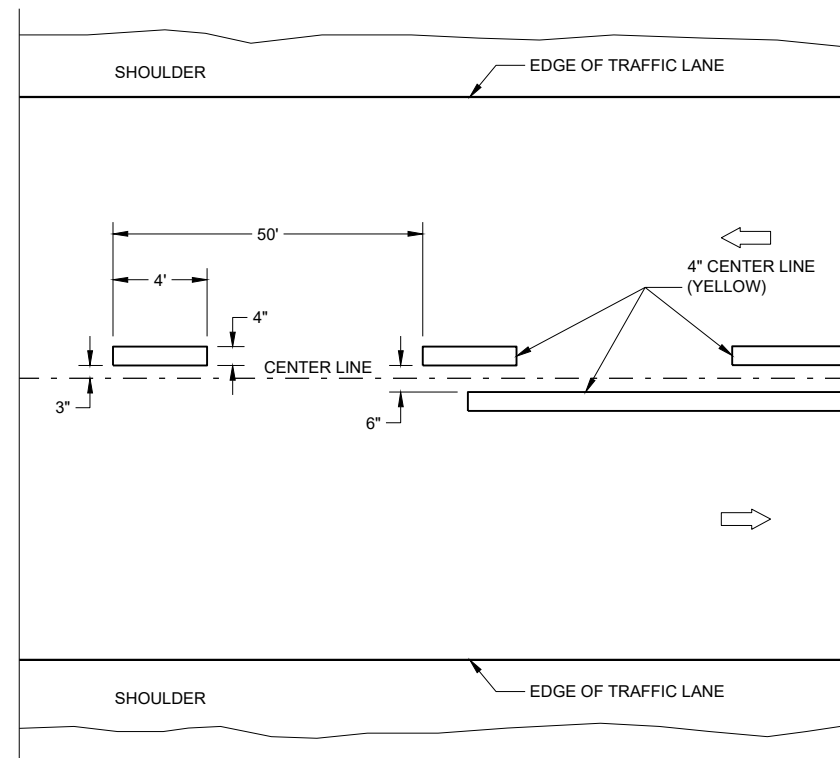
TEMPORARY BARRIER OFFSET FROM EDGELINE

GENERAL NOTES

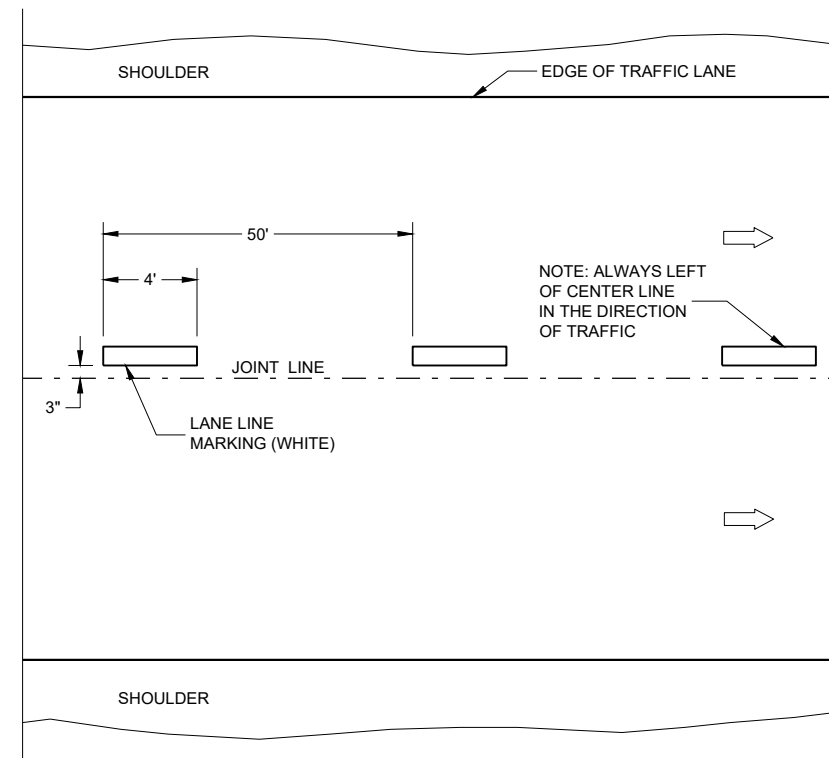
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

LEGEND

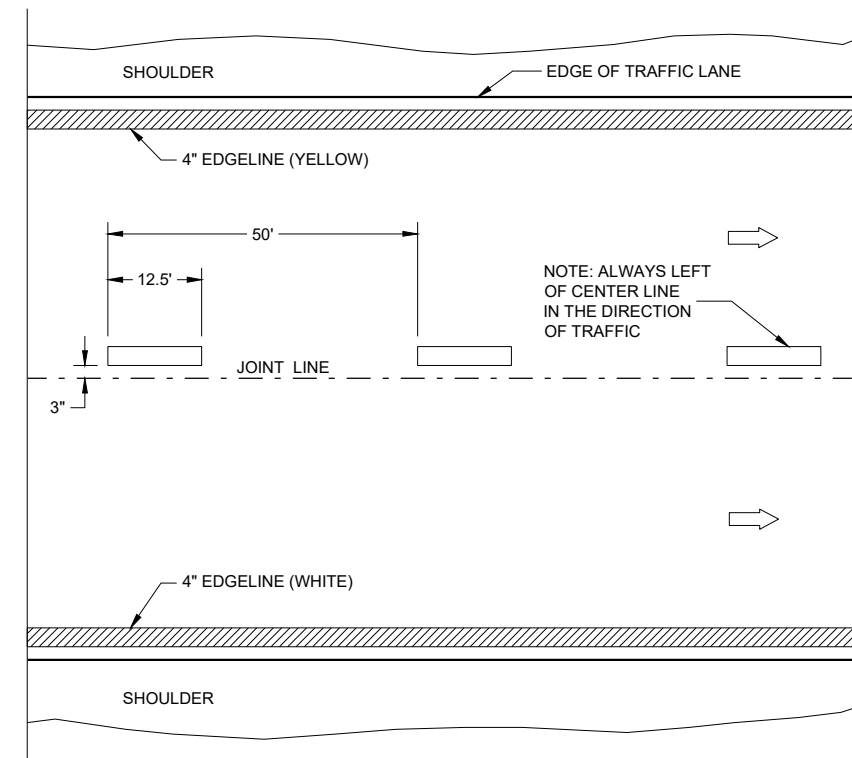
➡ DIRECTION OF TRAFFIC



TWO WAY TRAFFIC



ONE WAY TRAFFIC



FREEWAYS AND EXPRESSWAYS

TEMPORARY PAVEMENT MARKING

TEMPORARY LONGITUDINAL PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

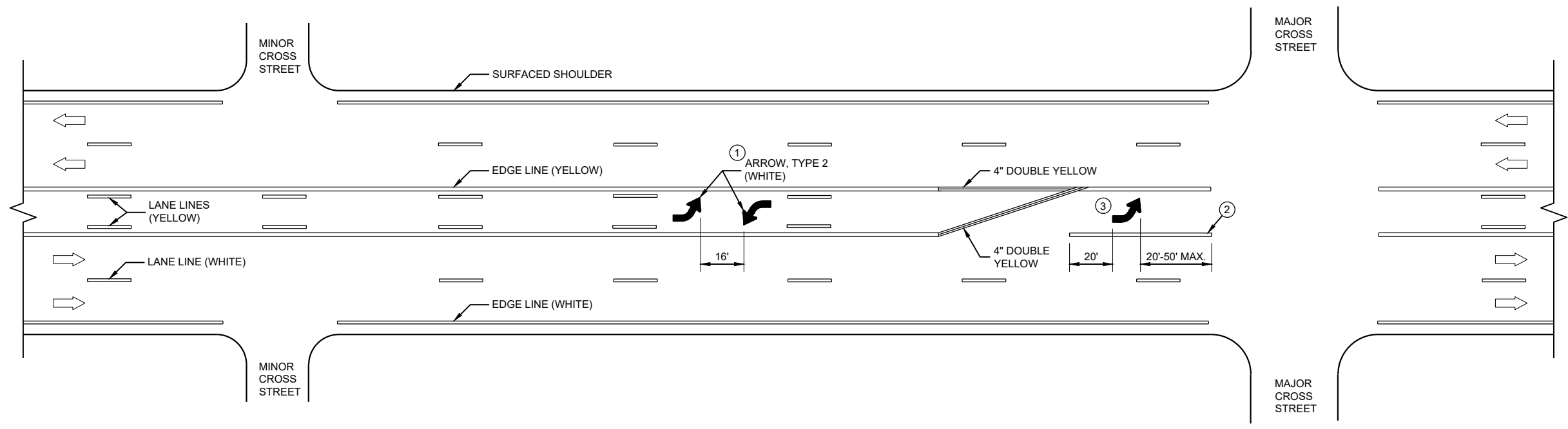
APPROVED
May 2022 /S/ Jeannie Silver
DATE STATEWIDE SIGNING AND MARKING ENGINEER

FHWA

GENERAL NOTES

- ① A SET OF ARROWS IS REQUIRED EVERY 400 FEET OR NEAR INTERSECTIONS OR DRIVEWAYS WITH TURNING TRAFFIC.
- ② 8" WHITE
- ③ TURN BAY LENGTH OF LESS THAN 48' DOES NOT REQUIRE PAVEMENT ARROWS OR TEXT.

➡ DIRECTION OF TRAFFIC



TWO WAY LEFT TURN LANE

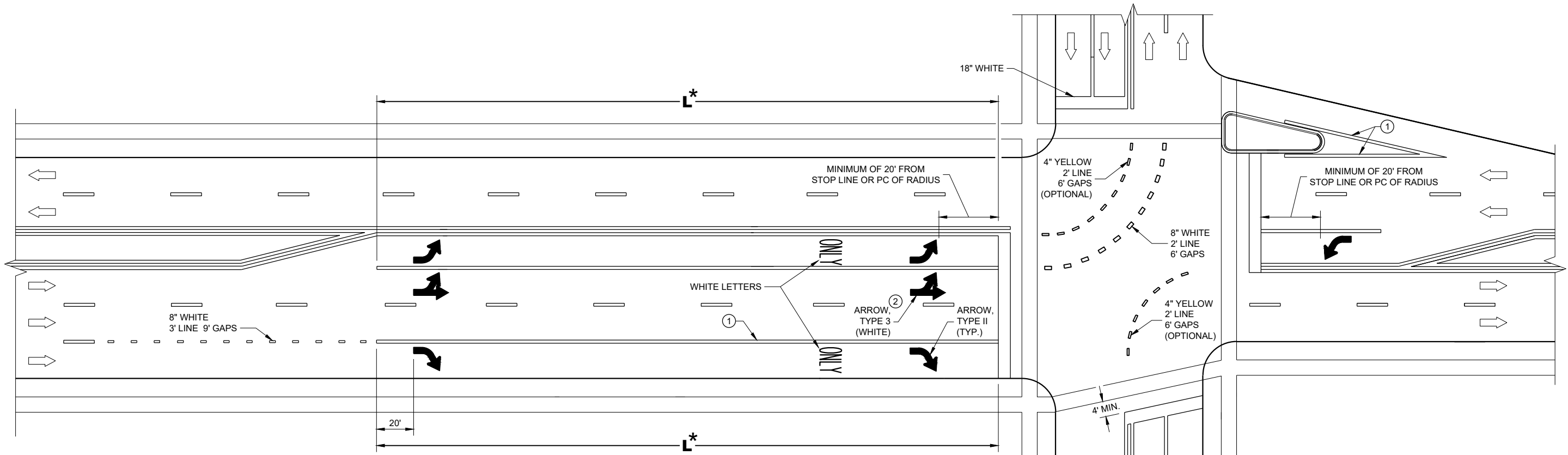
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SDD 15C08 - 21c

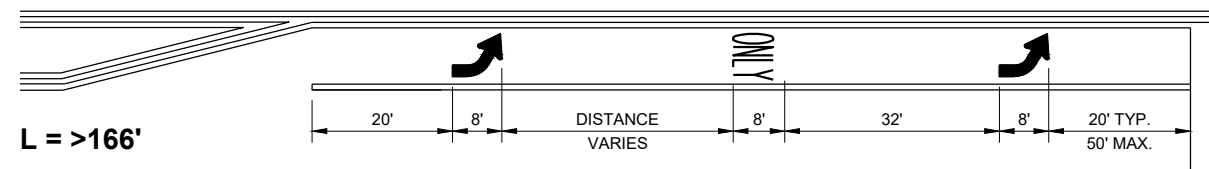
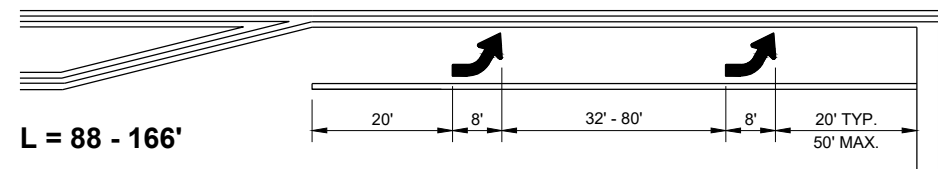
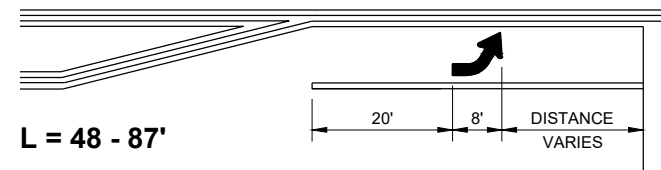
SDD 15C08 - 21c

<p>PAVEMENT MARKING (TURN LANES)</p>
<p>STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION</p>



TURN LANE OPTIONS

LENGTH OF TURN BAY (L) OF 0 - 47' DOES NOT REQUIRE PAVEMENT MARKING ARROWS OR WORDS



*(SEE TURN LANE OPTIONS FOR PLACEMENT OF PAVEMENT MARKING ARROWS AND WORDS)

GENERAL NOTES

- ① 8" WHITE
- ② QUANTITY AND LOCATION OF TYPE 3 ARROWS ARE THE SAME AS THE TYPE II ARROWS IN THE ADJACENT TURN LANE. FOR TURN LANES WITH A PHYSICAL SEPARATION IN THE SAME DIRECTION OF TRAVEL, THE ARROWS AND "ONLY" MARKING MAY BE ELIMINATED.






➡ DIRECTION OF TRAFFIC

L = LENGTH OF TURN BAY

PAVEMENT MARKING (TURN LANES)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

LEGEND

-  SIGN ON PORTABLE OR PERMANENT SUPPORT
-  TEMPORARY PORTABLE RUMBLE STRIP ARRAY
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS, DEVICES, AND LOCATION OF ALL FLAGGERS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGING

FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT REMOVE TEMPORARY PORTABLE RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.

- ① FOR MOVING WORK OPERATIONS, POST ADDITIONAL W20-7A FLAGGER SIGNS AT APPROXIMATELY 3,500' INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.
- ② SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

WHEN THE DISTANCE BETWEEN FLAGGERS EXCEEDS 2 MILES, A PILOT CAR IS REQUIRED. WHEN CURVES REDUCE SIGHT DISTANCE BELOW 400', A PILOT CAR IS REQUIRED.

TEMPORARY PORTABLE RUMBLE STRIPS

UTILIZE TEMPORARY PORTABLE RUMBLE STRIPS ON ALL FLAGGING OPERATIONS.

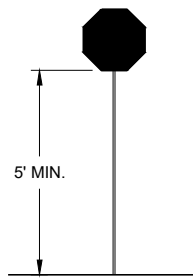
- ③ EACH TEMPORARY PORTABLE RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS PLACED TRANSVERSE ACROSS THE LANE AT THE LOCATIONS SHOWN. WITHIN EACH ARRAY, SPACING BETWEEN RUMBLE STRIPS SHALL BE 15 FEET ON CENTER

ONLY USE TEMPORARY PORTABLE RUMBLE STRIPS FROM THE APPROVED PRODUCTS LIST.

INSTALL TEMPORARY RUMBLE STRIPS PER MANUFACTURER'S RECOMMENDATIONS.

PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.

DO NOT INSTALL TEMPORARY PORTABLE RUMBLE STRIPS ON GRAVEL, MILLED SURFACES, OR ASPHALT THAT HAS BEEN PAVED LESS THAN 12 HOURS.



STOP/SLOW PADDLE ON SUPPORT STAFF

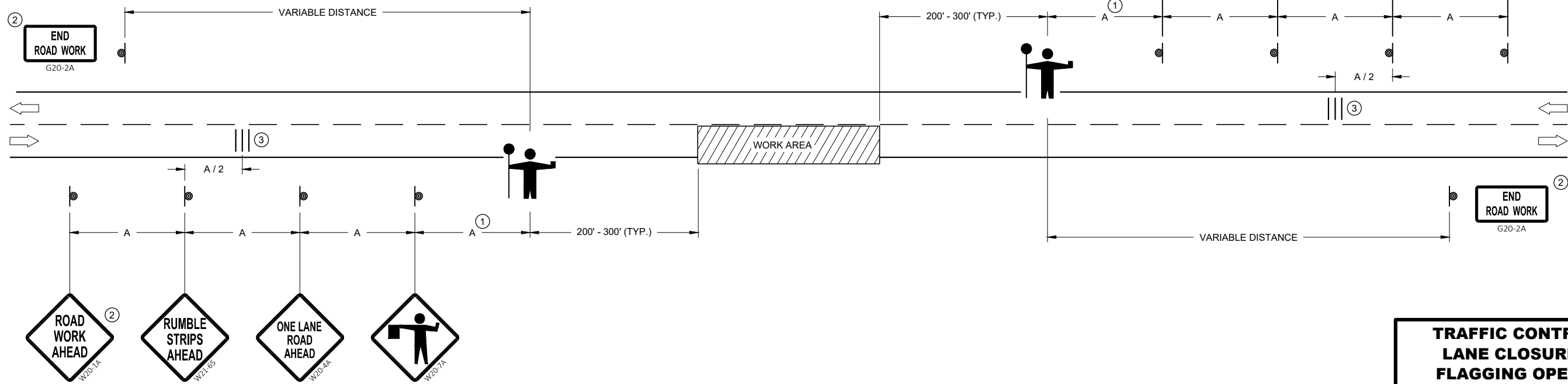
SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE

SPEED LIMIT	SPACING "A"
25-30 MPH	200'
35-40 MPH	350'
45-55 MPH	500'



W03-4

USE OF W03-4 SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7A AND W20-4A SIGNS, USING SPACING "A".

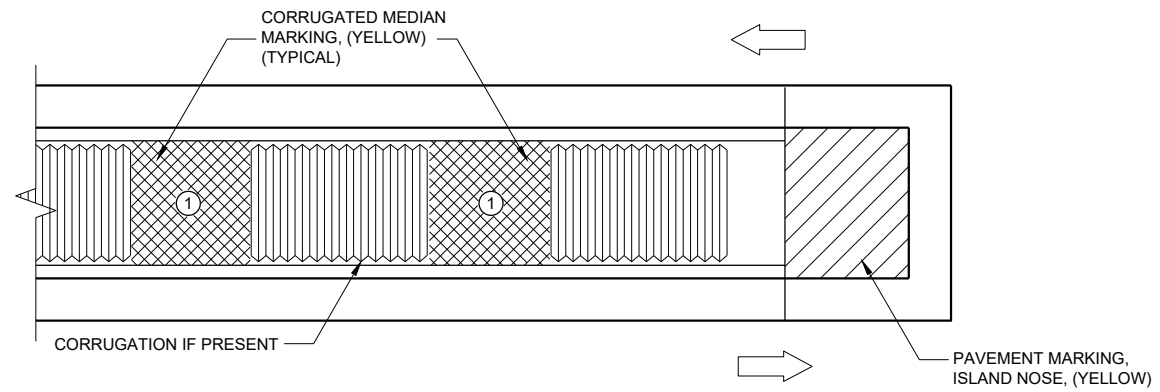


TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION

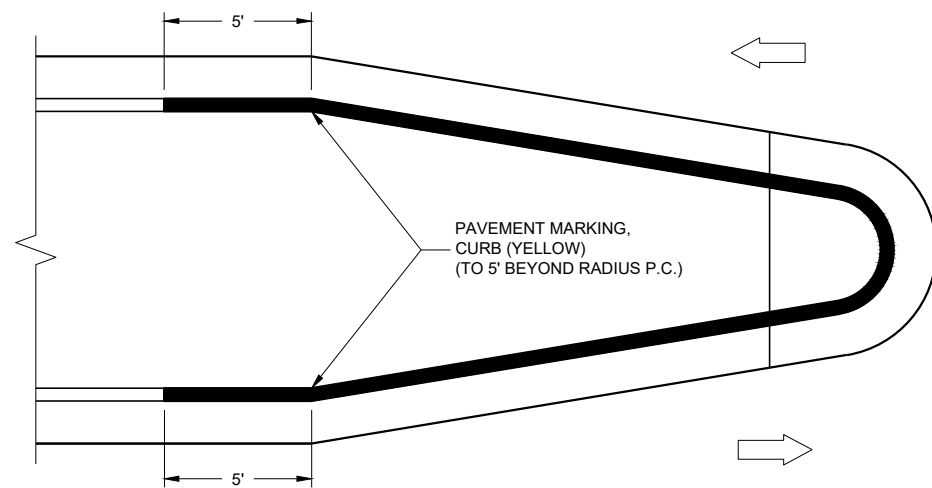
STATE OF WISCONSIN
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APPROVED
DATE: May 2022 /S/ Andrew Heidtke
WORK ZONE ENGINEER

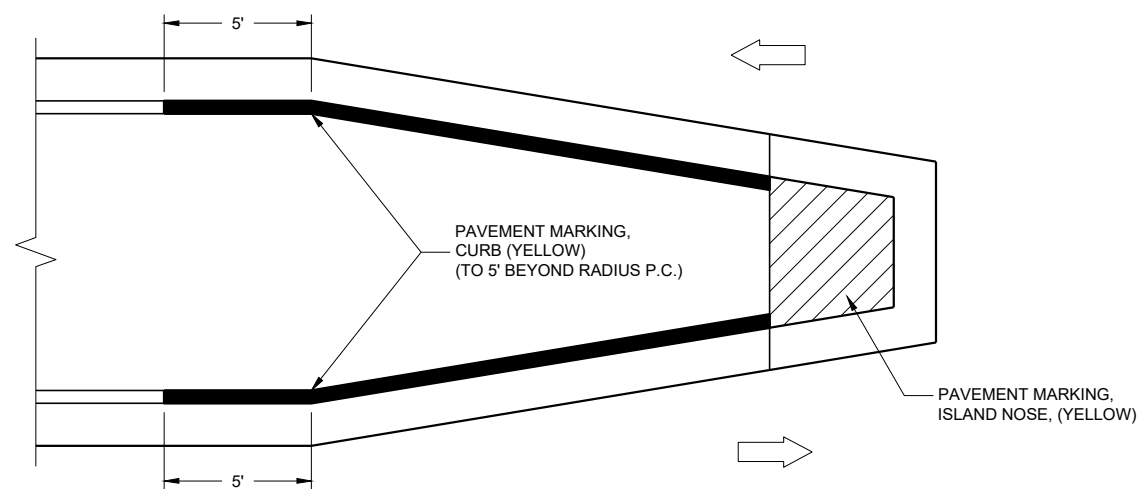
FHWA



MEDIAN ISLAND WITH SQUARE BLUNT NOSE



MEDIAN ISLAND WITH ROUND BLUNT NOSE







MEDIAN ISLAND WITH SLOPED NOSE

TYPICAL PLACEMENT OF PAVEMENT MARKING ON MEDIAN ISLANDS

GENERAL NOTES





WHEN CONCRETE CORRUGATED MEDIAN IS CONSTRUCTED TO SEPARATE TRAFFIC OPERATING IN THE OPPOSING DIRECTION, YELLOW PAVEMENT MARKING SHALL BE APPLIED TO THE FLAT PORTION OF THE CONCRETE CORRUGATED MEDIAN. THE ITEM OF PAVEMENT MARKING, CONCRETE CORRUGATED MEDIAN, WILL BE MEASURED IN PLACE AND ACCEPTED IN ACCORDANCE WITH THE CONTRACT AND PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT.

- ① APPLY PAVEMENT MARKING TO THE FLAT PORTION OF CORRUGATED MEDIAN.

-  ISLAND NOSE MARKING
-  CURB MARKING
-  CORRUGATED MEDIAN MARKING
-  DIRECTION OF TRAVEL

PAVEMENT MARKINGS, MEDIAN ISLAND NOSE	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2022 DATE	/S/ Jeannie Silver STATE SIGNING AND MARKING ENGINEER
<small>FHWA</small>	

LEGEND

- V1 LEAD VEHICLE
- V2 MARKING VEHICLE
- V3 SHADOW VEHICLE
-  TRUCK MOUNTED ATTENUATOR (TMA)
-  SIGN ON TEMPORARY SUPPORT
-  DIRECTION OF TRAFFIC
-  FLASHING ARROW PANEL (CAUTION)

GENERAL NOTES

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

ALL VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL OPERATING IN CAUTION MODE. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.

DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL AND HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.

THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS.

WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR LAY STATIONARY SIGNS AND SUPPORTS FLAT ON THE GRADE WITH UPRIGHTS ORIENTED PARALLEL TO AND DOWNSTREAM FROM TRAFFIC.

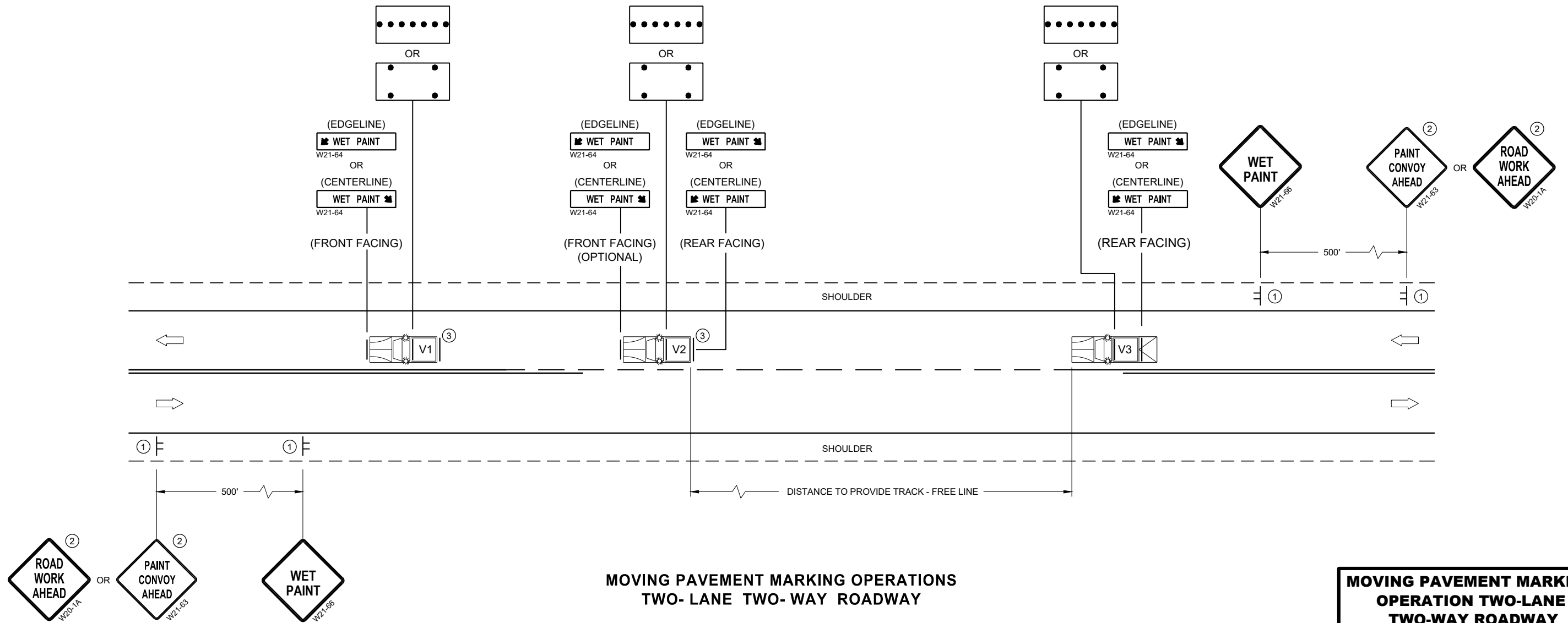
CONES SHOULD BE USED BETWEEN THE MARKING AND SHADOW VEHICLE AT 100 FOOT SPACING. CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.

CONES SHALL BE A MINIMUM OF 28" FOR WET PAVEMENT MARKING.

- ① SIGNS SHALL BE REPEATED APPROXIMATELY EVERY THREE MILES.
- ② IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1A OR W21-63 ARE NOT REQUIRED.
- ③ V1 AND V2 CAN BE SWITCHED SO THAT THE MARKER IS THE LEAD VEHICLE.

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**MOVING PAVEMENT MARKING OPERATIONS
TWO-LANE TWO-WAY ROADWAY**

SDD 15C19 - 07a

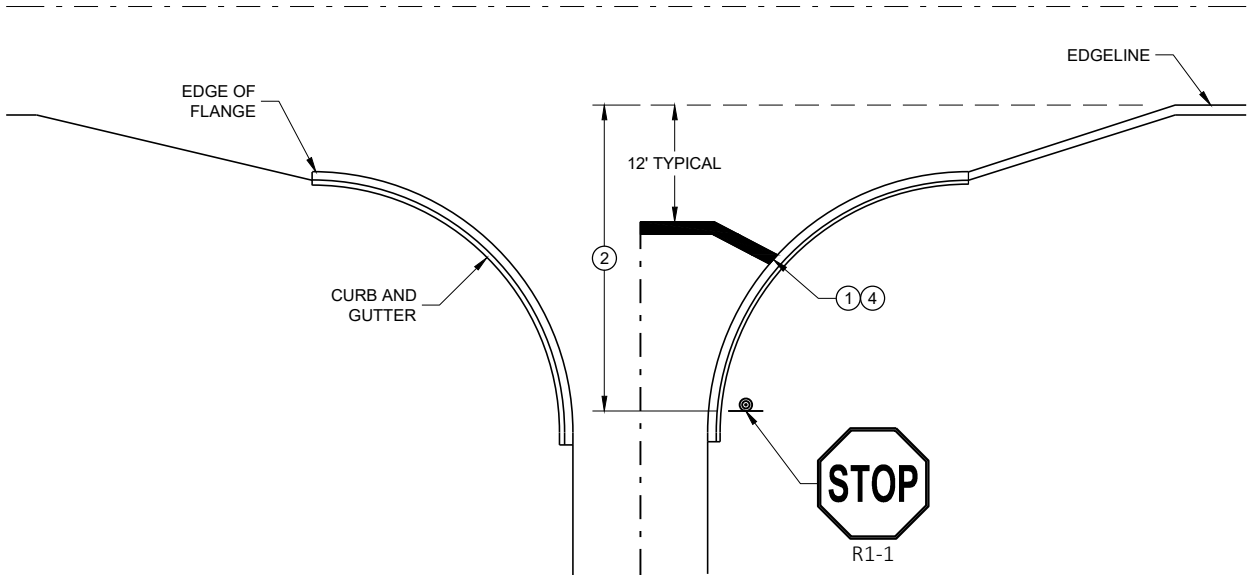
SDD 15C19 - 07a

MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

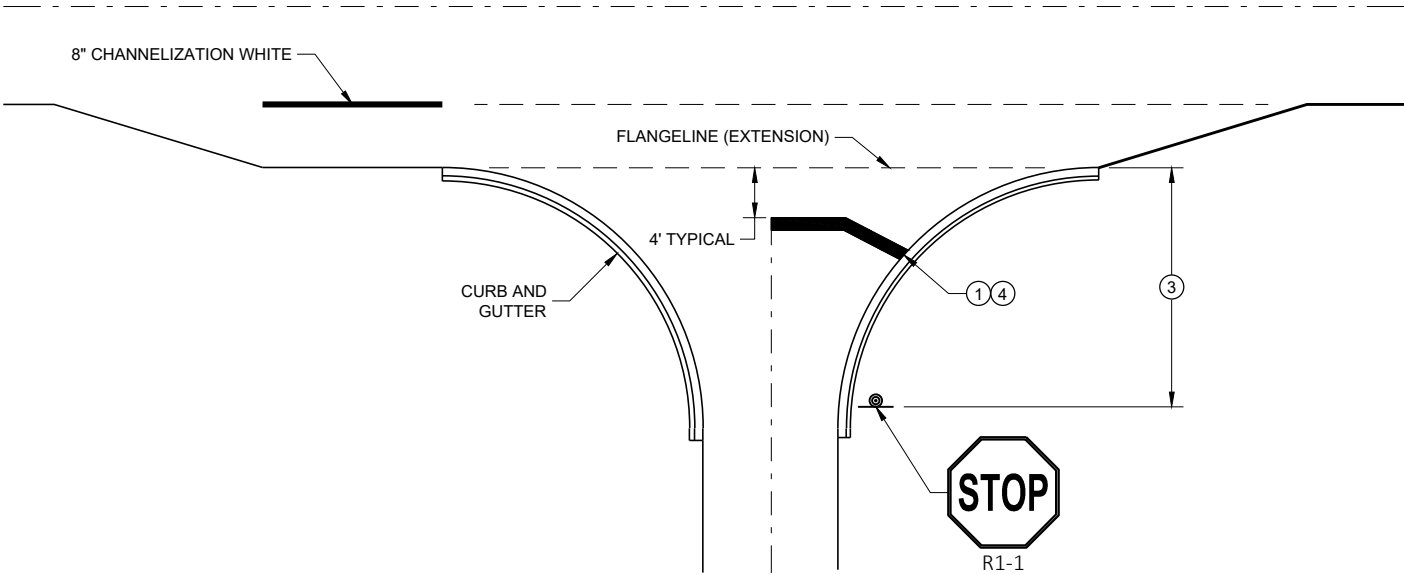
GENERAL NOTES

STOP SIGN SHALL BE PLACED A MINIMUM OF 6 FEET TO A MAXIMUM OF 50 FEET FROM THE EDGELINE LOCATION.

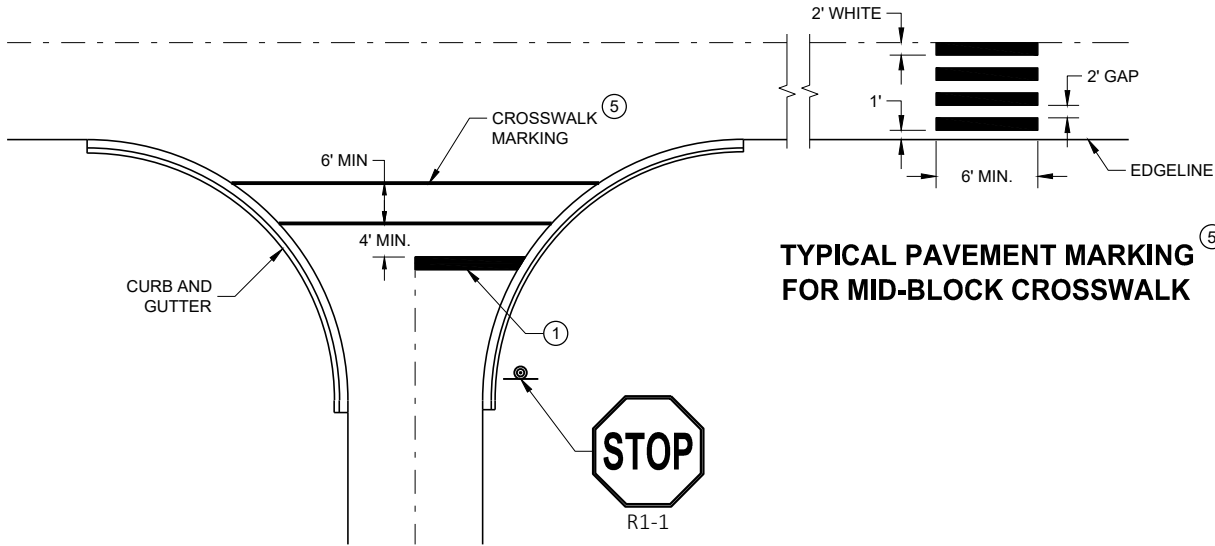
- ① 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE REGION MARKING ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
- ② NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGELINE.
- ③ NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGELINE EXTENSION.
- ④ MOVE CLOSER TO THE EDGE OF TRAVEL LINE AS NEEDED FOR VISIBILITY AND SIGHT LINES (NO CLOSER THAN 4 FEET).
- ⑤ LADDER BAR CROSSWALKS SHOULD ONLY BE USED FOR MID BLOCK CROSSINGS. USE 2 - 6" TRANSVERSE LINES INSTEAD.



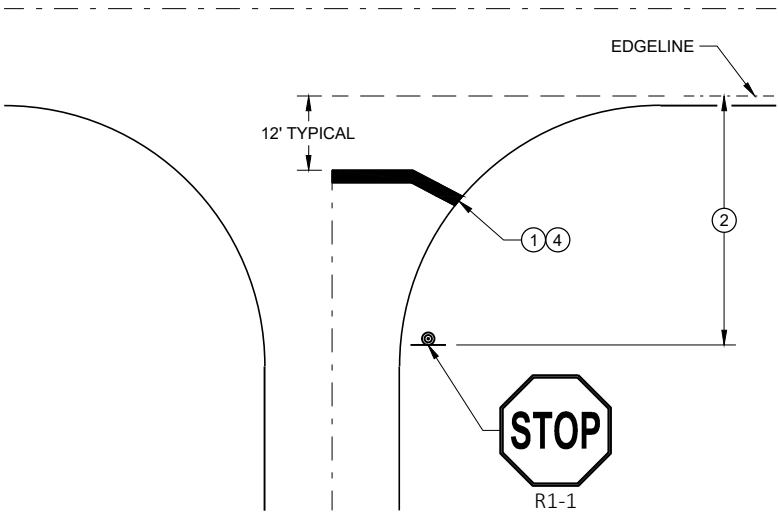
TYPICAL STOP LINE PAVEMENT MARKING WITH CURB AND GUTTER



TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH RIGHT TURN LANE



TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH CROSSWALK MARKING



TYPICAL STOP LINE PAVEMENT MARKING WITHOUT CURB AND GUTTER

STOP LINE AND CROSSWALK PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2019 /S/ Matthew Rauch
DATE STATE SIGNING AND MARKING ENGINEER

FHWA

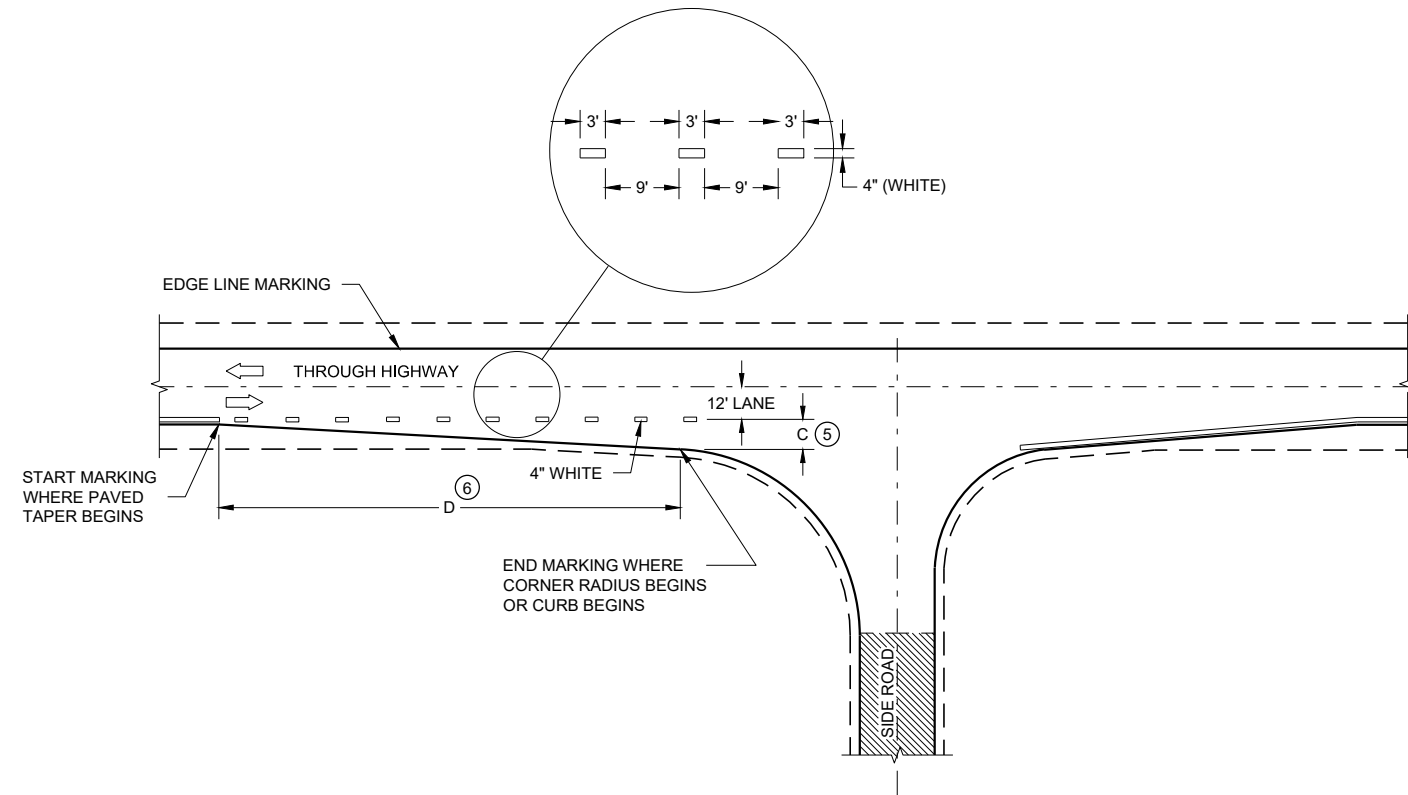
GENERAL NOTES

OMIT EDGE LINES THROUGH INTERSECTIONS. CONTINUE EDGE LINES THROUGH DRIVEWAYS.

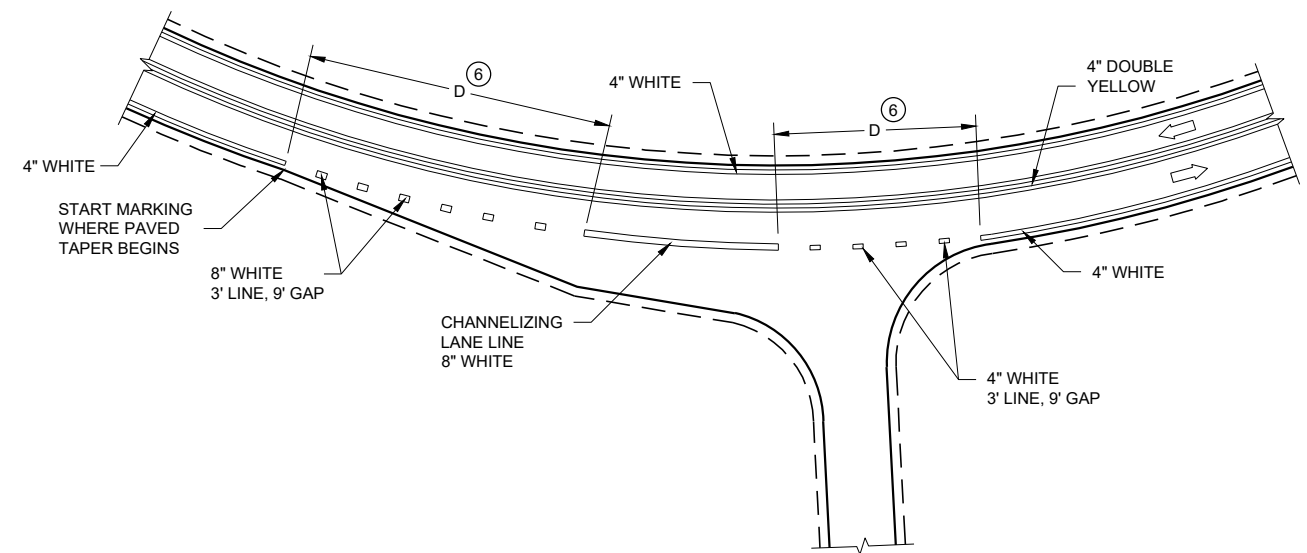
- ① WHEN DISTANCE "A" IS LESS THAN 250 FEET, OMIT LANE LINE.
- ② WHEN DISTANCE "B" IS LESS THAN 100 FEET, OMIT CHANNELIZING LANE LINE.
- ③ BARRIER LINE ENDS AT SIDE ROAD PAVEMENT / SURFACE EDGE EXTENSION.
- ④ BARRIER LINE STARTS 500 FEET PRIOR TO THE BYPASS TAPER.
- ⑤ WHEN DISTANCE "C" IS LESS THAN 4 FEET, OMIT DOTTED EXTENSION.
- ⑥ WHEN DISTANCE "D" IS LESS THAN 50 FEET, OMIT DOTTED EXTENSION.

LEGEND

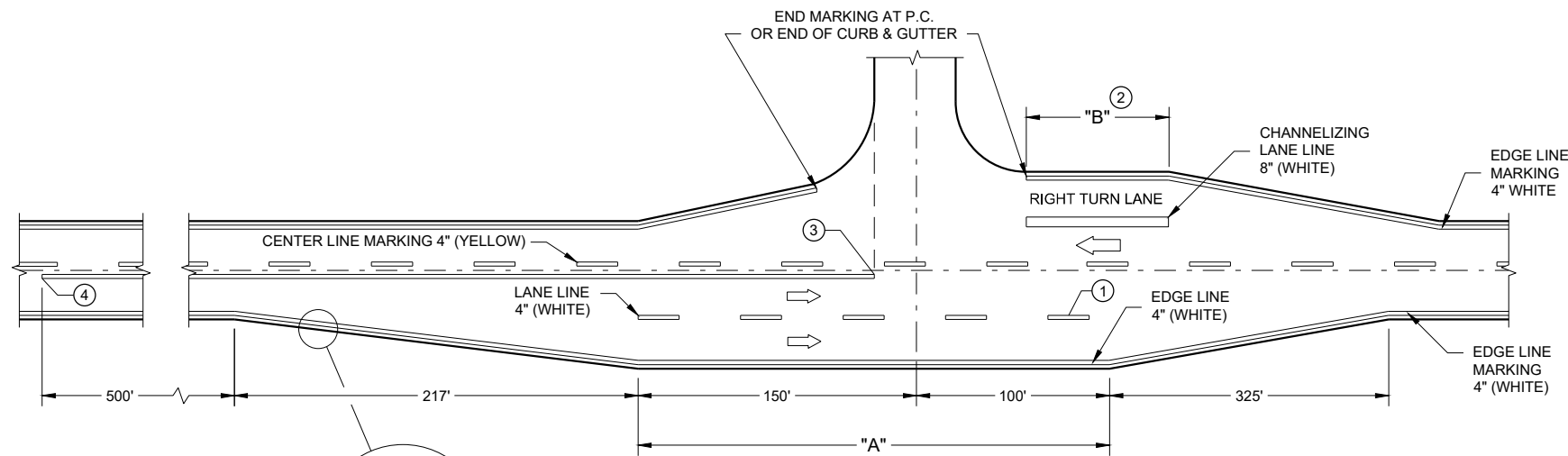
➡ DIRECTION OF TRAVEL



MINOR INTERSECTION

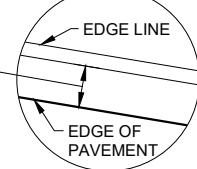


INTERSECTION ON OUTSIDE OF CURVE



**MAJOR INTERSECTIONS
(INTERSECTION WITH FULL RIGHT TURN LANE OR BYPASS LANE)**

BYPASS LANE PAVED SHOULDER WIDTH (AS SHOWN ELSEWHERE IN PLANS) - PLUS 2 INCHES



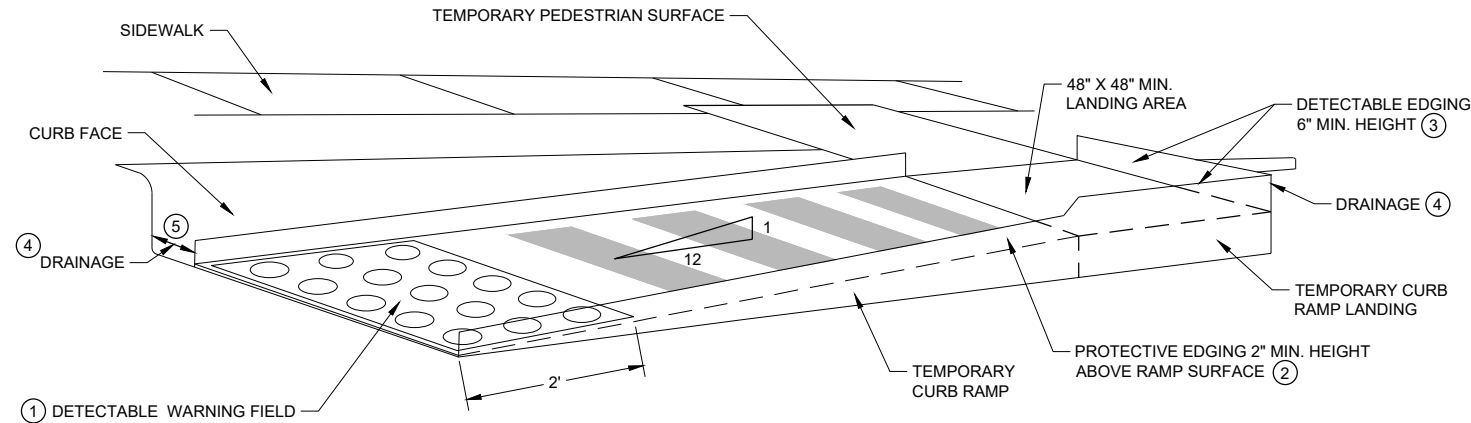
**PAVEMENT MARKING
(INTERSECTIONS)**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

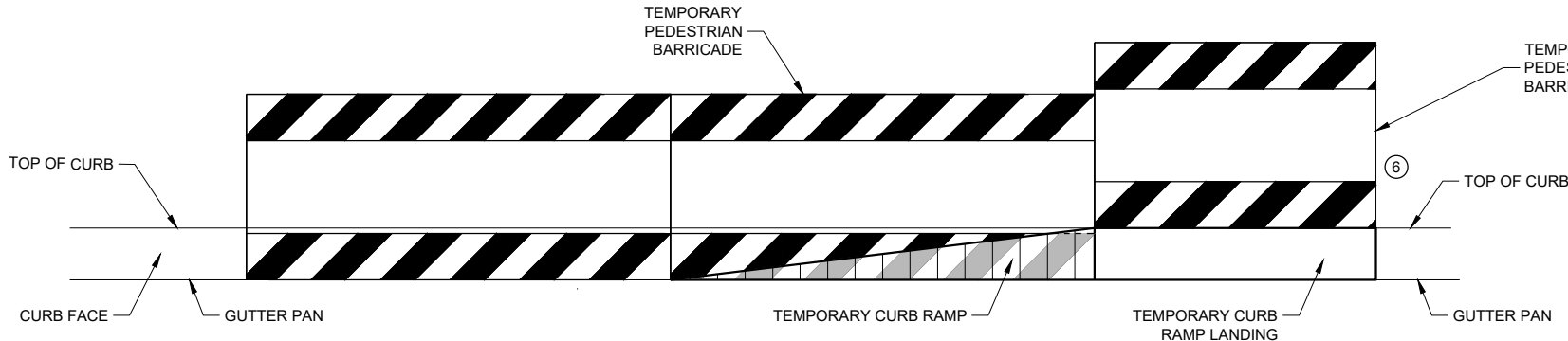
GENERAL NOTES

CURB RAMPS SHALL BE 48" MIN. WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE.
 CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.
 CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.
 LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.
 CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES MAY BE VERTICAL UP TO 1/4" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".

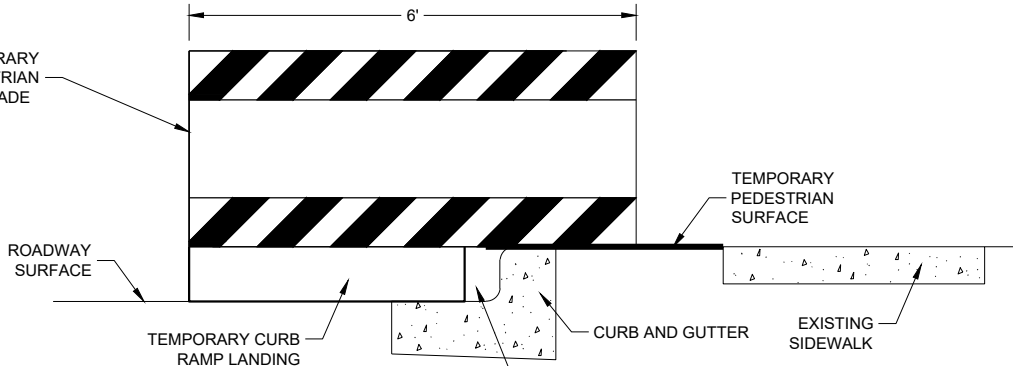
- ① INSTALL CONTRASTING TEMPORARY DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS, AS SHOWN IN THE PLANS.
- ② PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- ③ DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- ④ DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ⑤ 6" MINIMUM BETWEEN CURB FACE AND EDGE OF RAMP
- ⑥ IF ONLY PART OF THE END PANEL OF TEMPORARY PEDESTRIAN BARRICADE PANEL IS NEEDED, EXTEND EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL HERE.



PERSPECTIVE VIEW



FRONT VIEW









SIDE VIEW

TEMPORARY CURB RAMP PARALLEL TO CURB

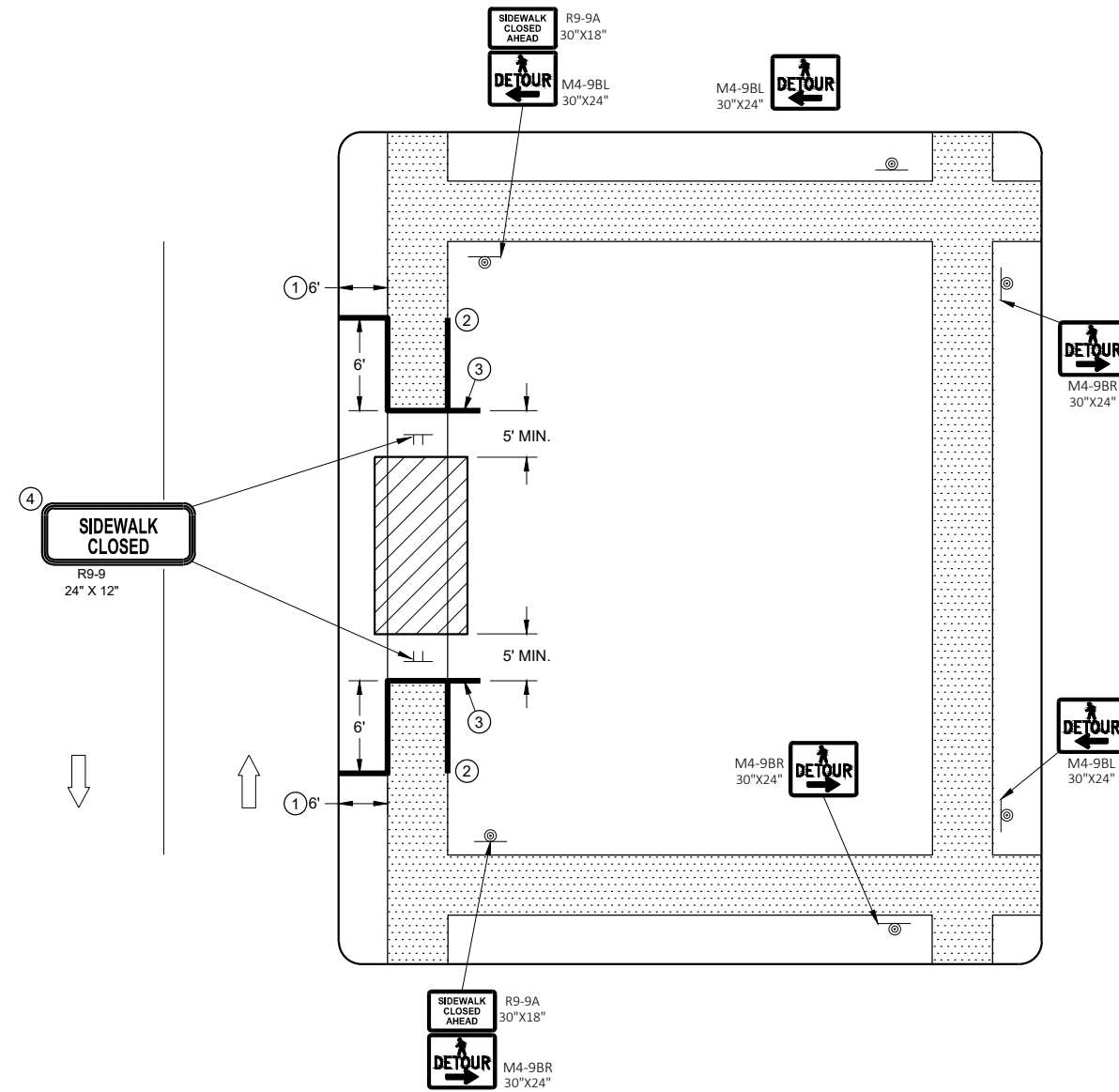
<p>TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION</p>
<p>STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION</p>

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  SIGN ON TEMPORARY SUPPORT
-  UNDER PEDESTRIAN TRAFFIC
-  WORK AREA
-  TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC

GENERAL NOTES

- SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.
- WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.
- SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.
- ① IF TERRACE IS LESS THAN 6 FEET WIDE, OMIT TEMPORARY PEDESTRIAN BARRICADE FROM THE SIDEWALK TO THE CURB.
 - ② PLACE BARRICADE CLOSURE SO THAT THE TEMPORARY PEDESTRIAN BARRICADE END IS AT THE LAST OPEN SIDEWALK ACCESS TO RESIDENCES OR BUSINESSES BEFORE THE SIDEWALK CLOSURE.
 - ③ IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
 - ④ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.



SIDEWALK DETOUR, SIDEWALK ONLY ON ONE SIDE

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

IF PEDESTRIAN PUSH BUTTONS ARE PRESENT ON THE EXISTING FACILITY, ENSURE THEY ARE MAINTAINED/ACCESSIBLE FOR PEDESTRIAN USE THROUGHOUT THE TEMPORARY PEDESTRIAN ACCOMMODATIONS.











SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

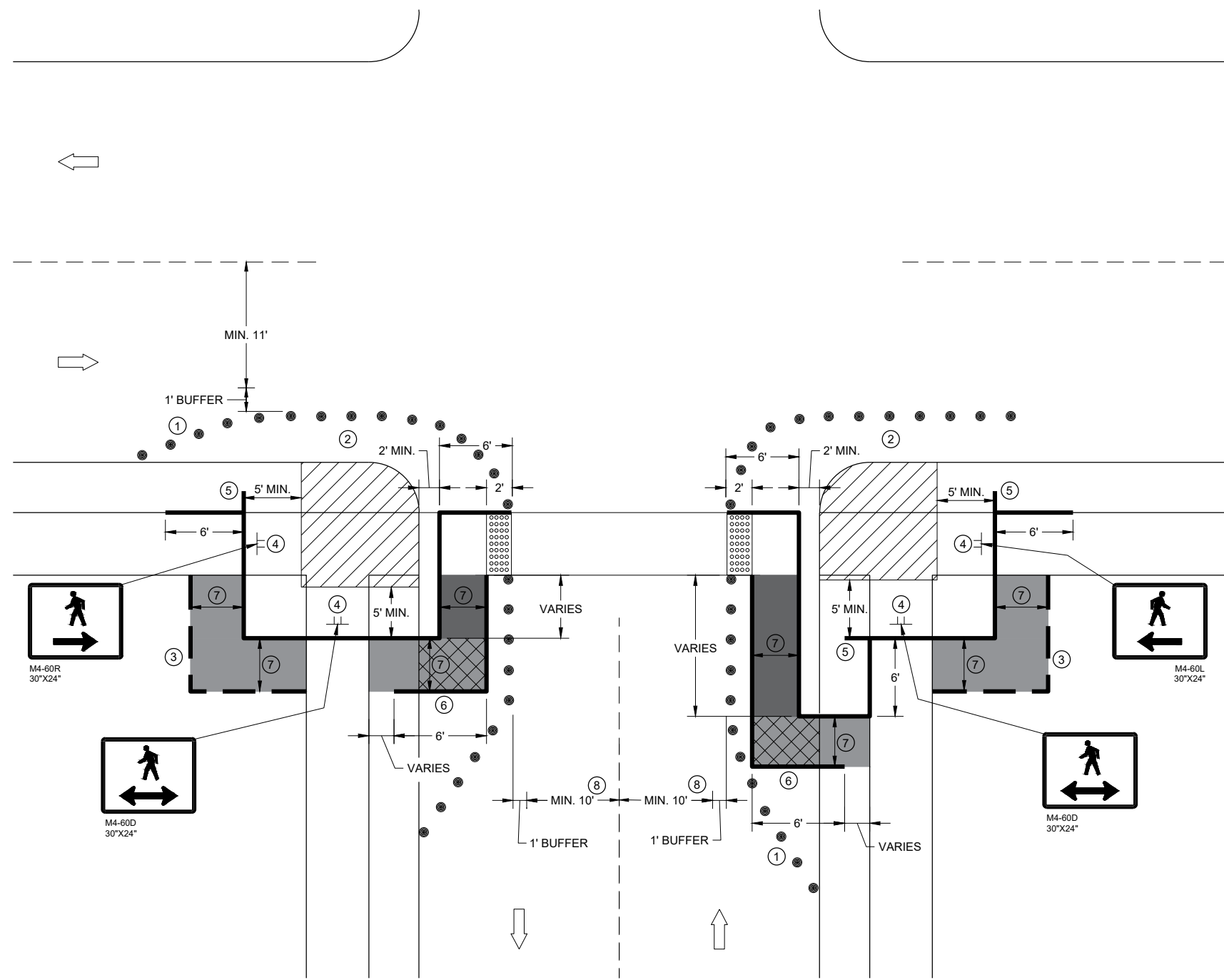
TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG

WHEN TEMPORARY PEDESTRIAN BARRICADE RUNS PARALLEL ALONG THE SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND PROPER BUFFER SPACE REQUIRED.
- ② PROVIDE ADEQUATE SPACE FOR CONTRACTOR OPERATIONS
- ③ USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
- ④ MOUNTING HEIGHT OF 5 FEET FROM SIDEWALK SURFACE TO BOTTOM OF SIGN.
- ⑤ PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL IN THE SIDEWALK TERRACE.
- ⑥ IF TEMPORARY PEDESTRIAN BARRICADE DOES NOT REACH THE FACE OF THE CURB, USE AN ADDITIONAL PANEL AND EXTEND INTO THE TERRACE.
- ⑦ 4 FEET MINIMUM, 5 FEET DESIRABLE
- ⑧ IF MINIMUM LANE WIDTHS CAN'T BE ATTAINED, CURB RAMPS MAY NEED TO BE CONSTRUCTED AT SEPARATE TIMES.

LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL DRUM
-  WORK AREA
-  TEMPORARY CURB RAMP
-  TEMPORARY PEDESTRIAN SURFACE "A"
-  TEMPORARY PEDESTRIAN SURFACE "B"
-  TEMPORARY DETECTABLE WARNING FIELD
-  TEMPORARY PEDESTRIAN BARRICADE
-  OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC



**CURB RAMP PEDESTRIAN TRAFFIC CONTROL
SIDEWALK ON SINGLE SIDE**

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

6

6

SDD 15D30 - 07i

SDD 15D30 - 07i

GENERAL NOTES

IF PEDESTRIAN PUSH BUTTONS ARE PRESENT ON THE EXISTING FACILITY, ENSURE THEY ARE MAINTAINED/ACCESSIBLE FOR PEDESTRIAN USE THROUGHOUT THE TEMPORARY PEDESTRIAN ACCOMMODATIONS.











SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

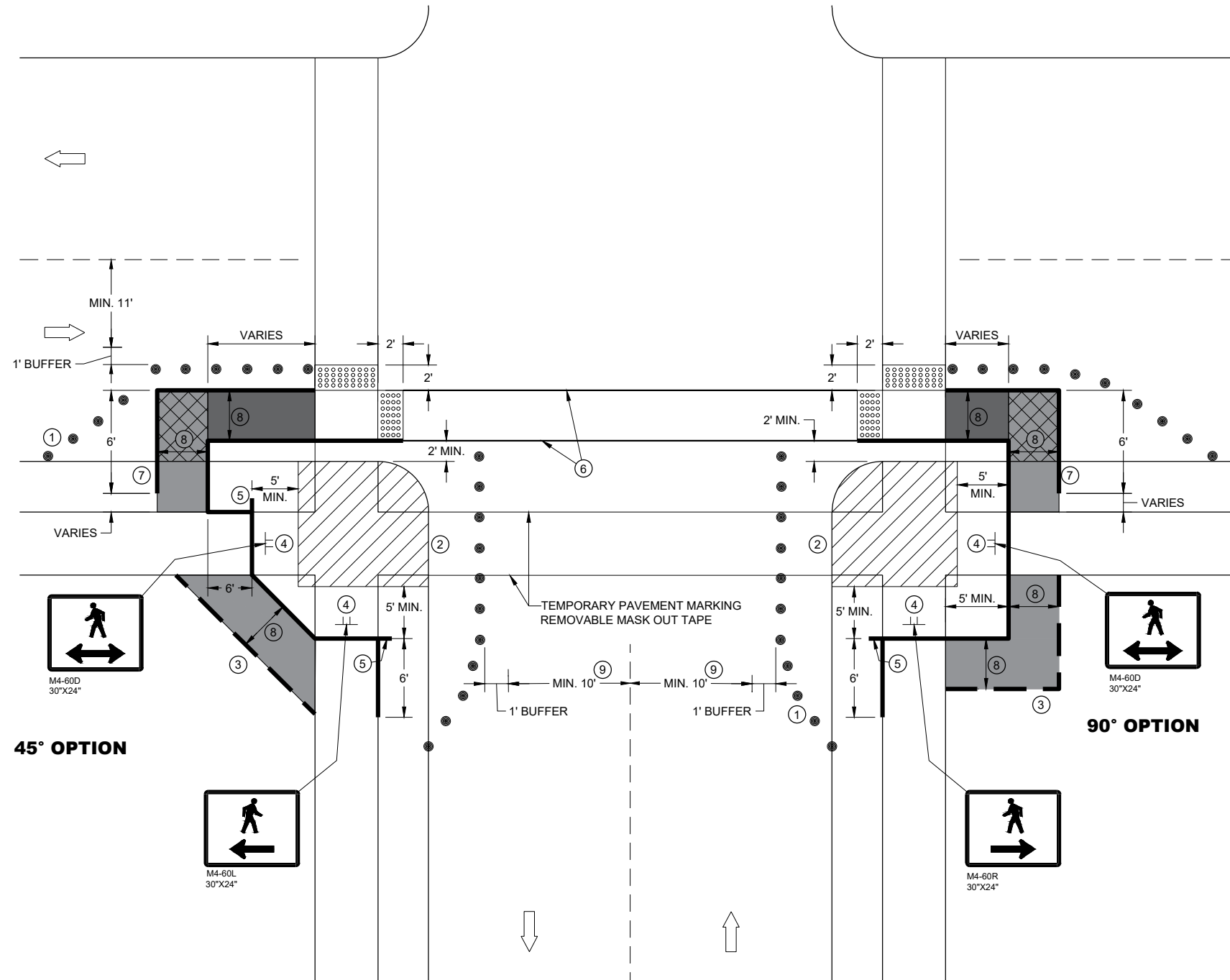
TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG

WHEN TEMPORARY PEDESTRIAN BARRICADE RUNS PARALLEL ALONG THE SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND PROPER BUFFER SPACE REQUIRED.
- ② PROVIDE ADEQUATE SPACE FOR CONTRACTOR OPERATIONS
- ③ USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
- ④ MOUNTING HEIGHT OF 5 FEET FROM SIDEWALK SURFACE TO BOTTOM OF SIGN.
- ⑤ PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL IN THE SIDEWALK TERRACE.
- ⑥ WHITE 6" TEMPORARY PAVEMENT MARKING
- ⑦ IF TEMPORARY PEDESTRIAN BARRICADE DOES NOT REACH THE FACE OF THE CURB, USE AN ADDITIONAL PANEL AND EXTEND INTO THE TERRACE.
- ⑧ 4 FEET MINIMUM, 5 FEET DESIRABLE
- ⑨ IF MINIMUM LANE WIDTHS CAN'T BE ATTAINED, CURB RAMPS MAY NEED TO BE CONSTRUCTED AT SEPARATE TIMES.

LEGEND

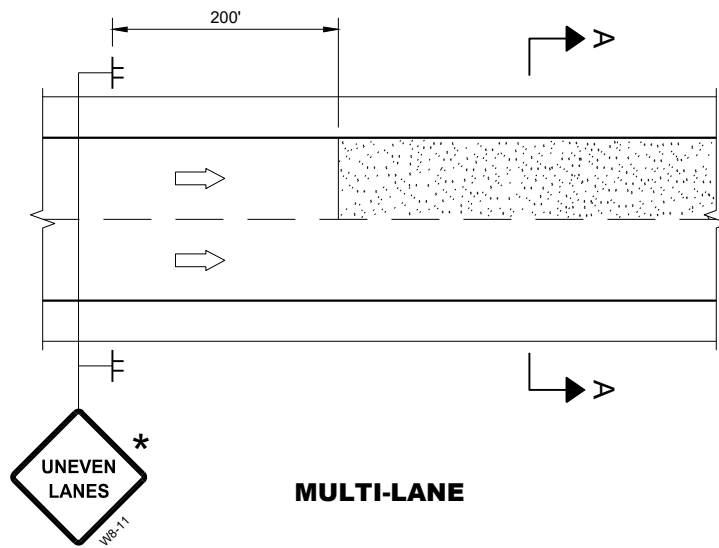
-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL DRUM
-  WORK AREA
-  TEMPORARY CURB RAMP
-  TEMPORARY PEDESTRIAN SURFACE "A"
-  TEMPORARY PEDESTRIAN SURFACE "B"
-  TEMPORARY DETECTABLE WARNING FIELD
-  TEMPORARY PEDESTRIAN BARRICADE
-  OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC



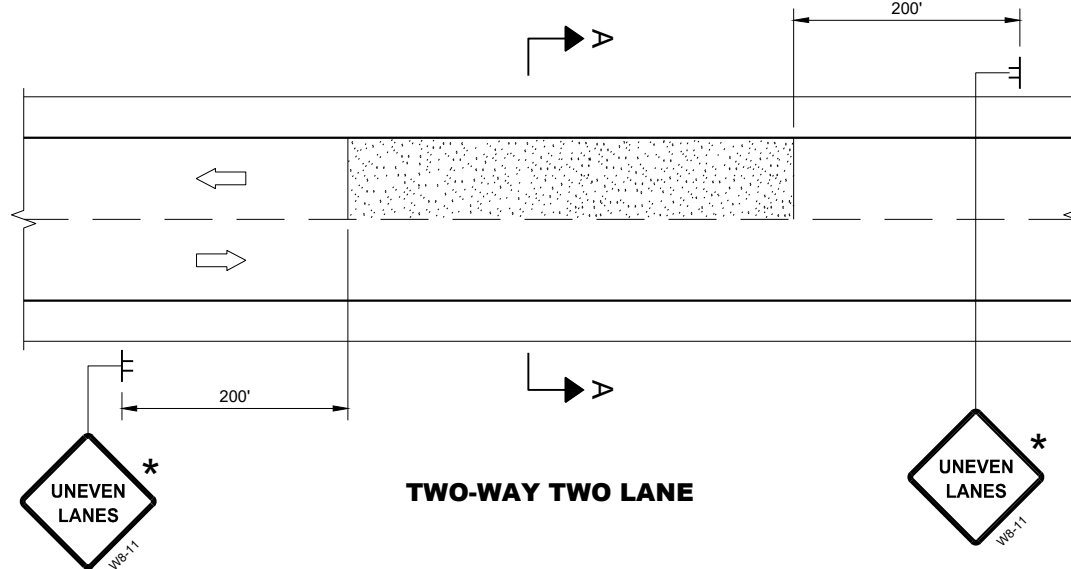
CURB RAMP PEDESTRIAN TRAFFIC CONTROL

**TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION**

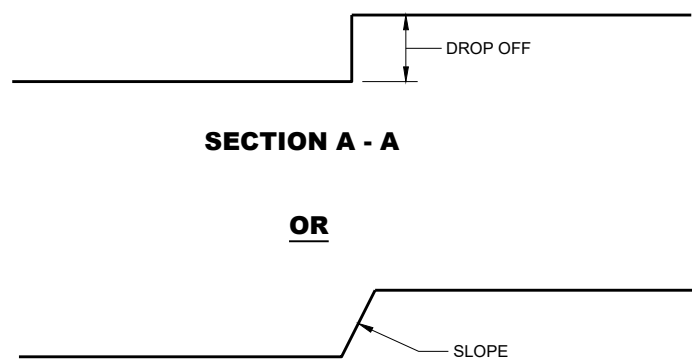
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



MULTI-LANE



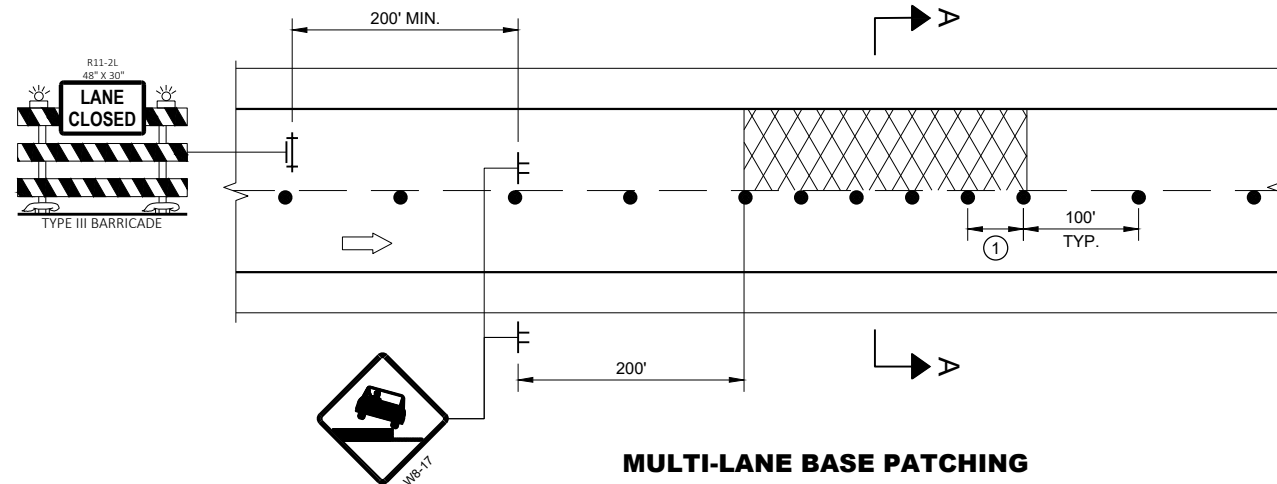
TWO-WAY TWO LANE



SECTION A - A

OR

SECTION A - A



MULTI-LANE BASE PATCHING

ADJACENT LANE DROP-OFFS

GENERAL NOTES

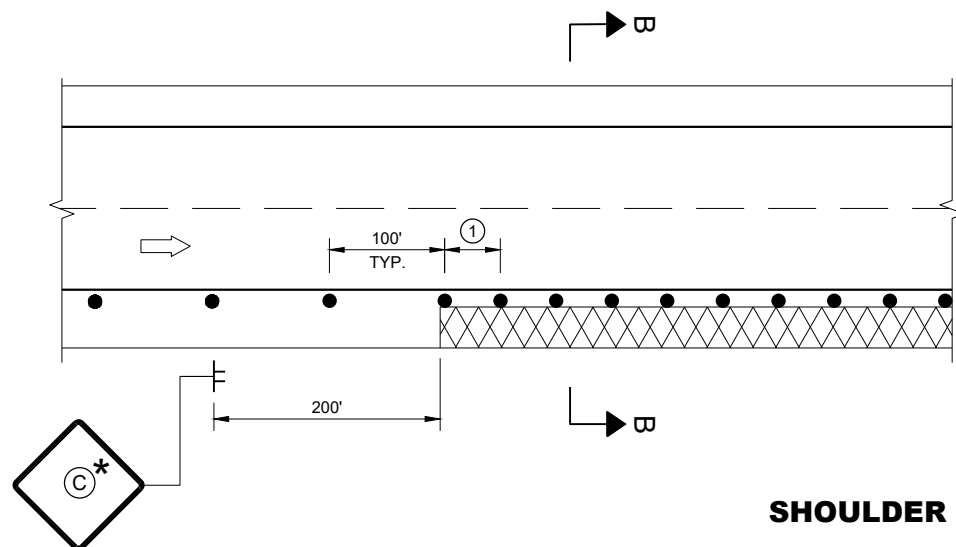
- FOR SPOT LOCATIONS USE ENGINEERING JUDGEMENT WHEN PLACING ADDITIONAL SIGNS.
- ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.
- "WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.
- WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.
- * IF THE DROP-OFF IS CONTINUOUS ALONG THE PROJECT, PLACE ADDITIONAL SIGNS EVERY 1 MILE AND AFTER EVERY ENTRANCE RAMP.
- ① USE CLOSER SPACING WHEN DELINEATING DROP-OFF.

LEGEND

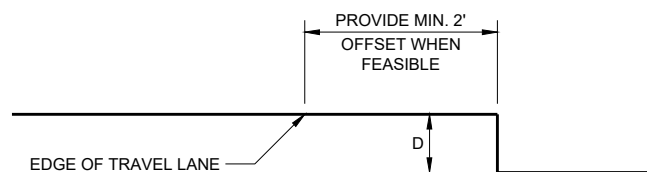
- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL DRUM
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- DIRECTION OF TRAFFIC
- WORK AREA WITH DROP-OFF
- MILLED SURFACE

6

6



SHOULDER DROP-OFFS



SECTION B - B

D	SIGN (C)
< 2" WITH A SLOPE STEEPER THAN 3:1	LOW SHOULDER WO8-9
2" < 6" WITH A SLOPE STEEPER THAN 3:1	SHOULDER DROP - OFF W8-9A PROVIDE A 3:1 OR FLATTER SLOPE OF MATERIAL ADJACENT TO THE PAVEMENT

SDD 15D39 - 02

SDD 15D39 - 02

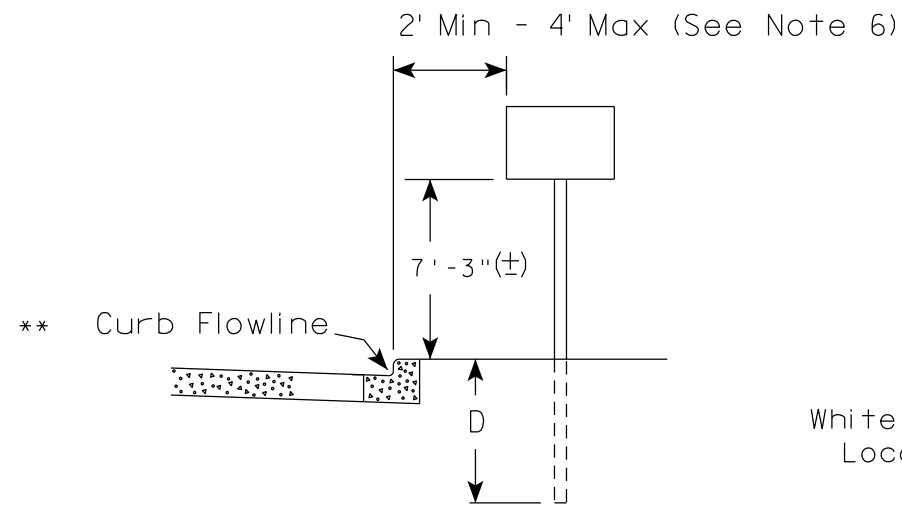
**TRAFFIC CONTROL,
DROP-OFF SIGNING**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

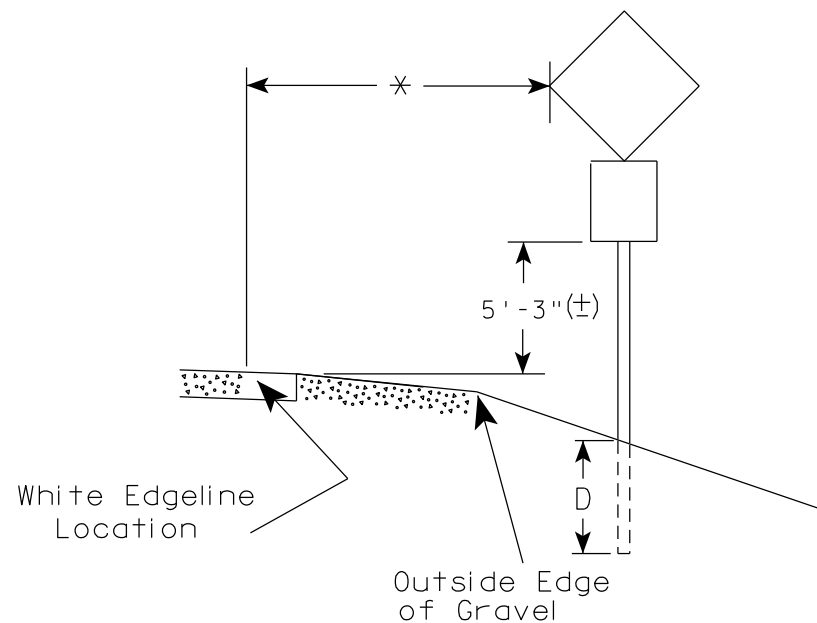
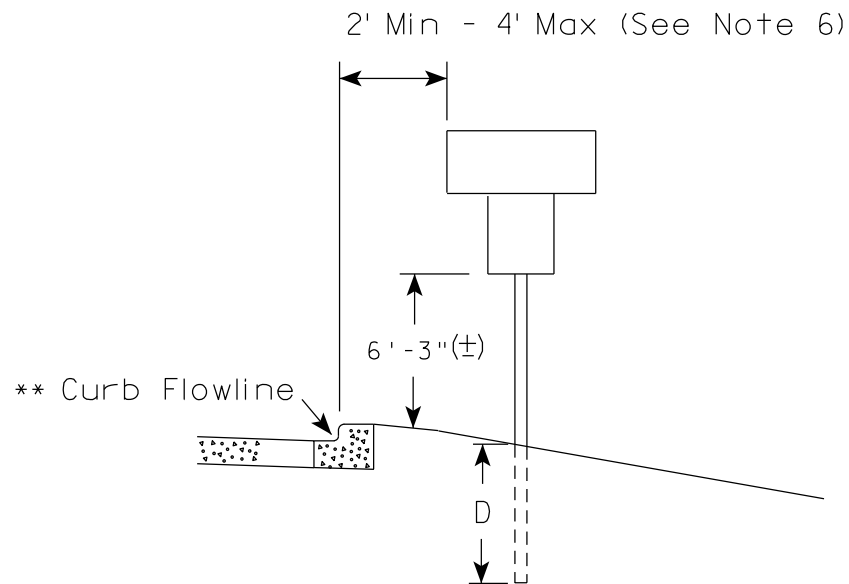
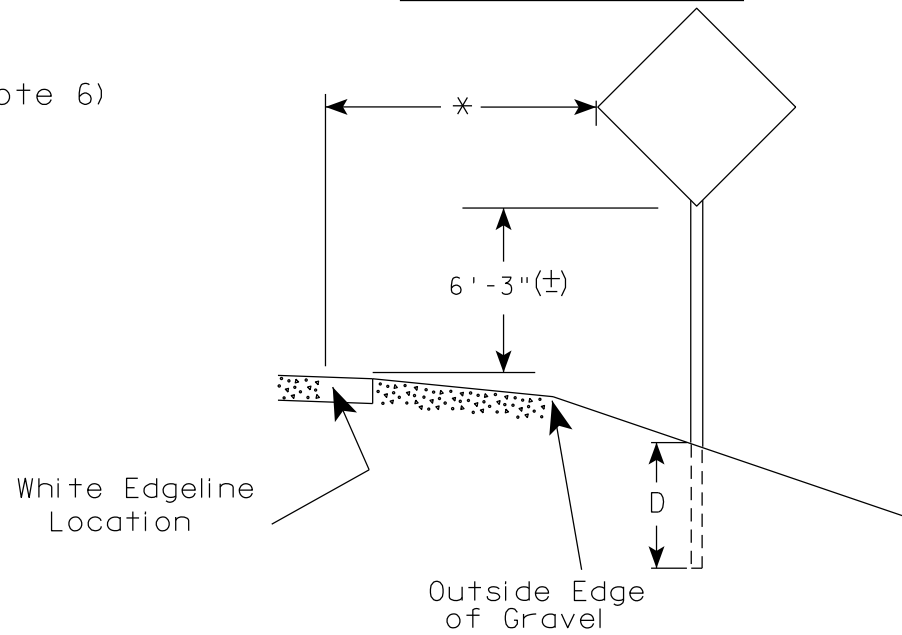
APPROVED
March 2018 /S/ Andrew Heidtke
DATE DATE WORK ZONE ENGINEER

FHWA

URBAN AREA



RURAL AREA (See Note 2)



POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

- GENERAL NOTES**
1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
 2. If signs are mounted on or behind barrier wall, see A4-10 sign plate.
The Double Arrow sign (W12-1D) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).
 3. For expressways and freeways, mounting height is 7'- 3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
 4. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±).
 5. Offset distance shall be consistent with existing signs or consistent throughout length of project.
 6. The (±) tolerance for mounting height is 3 inches.
 7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the Engineer.

* * The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

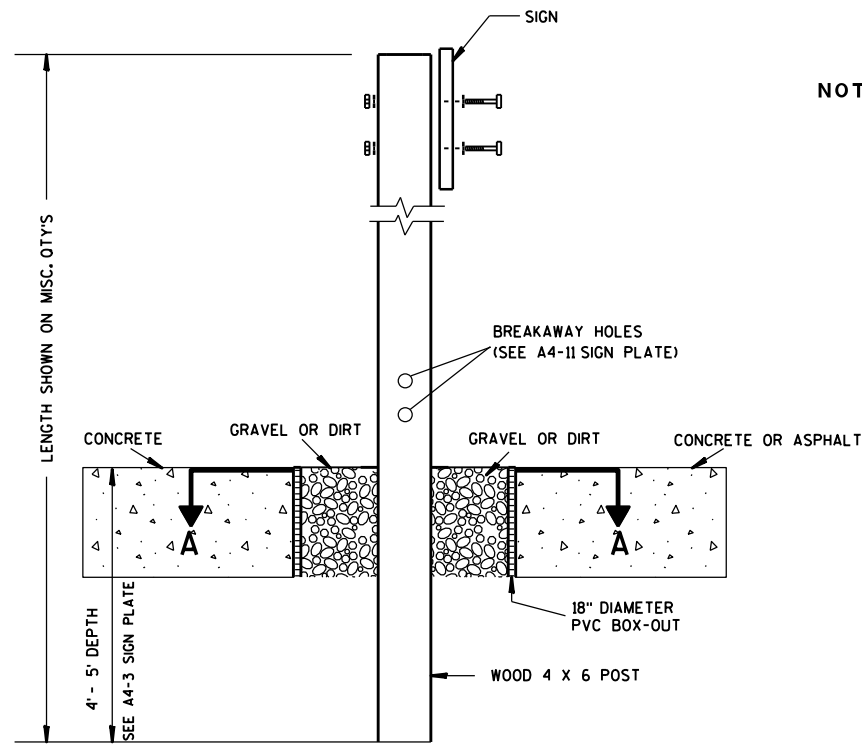
* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

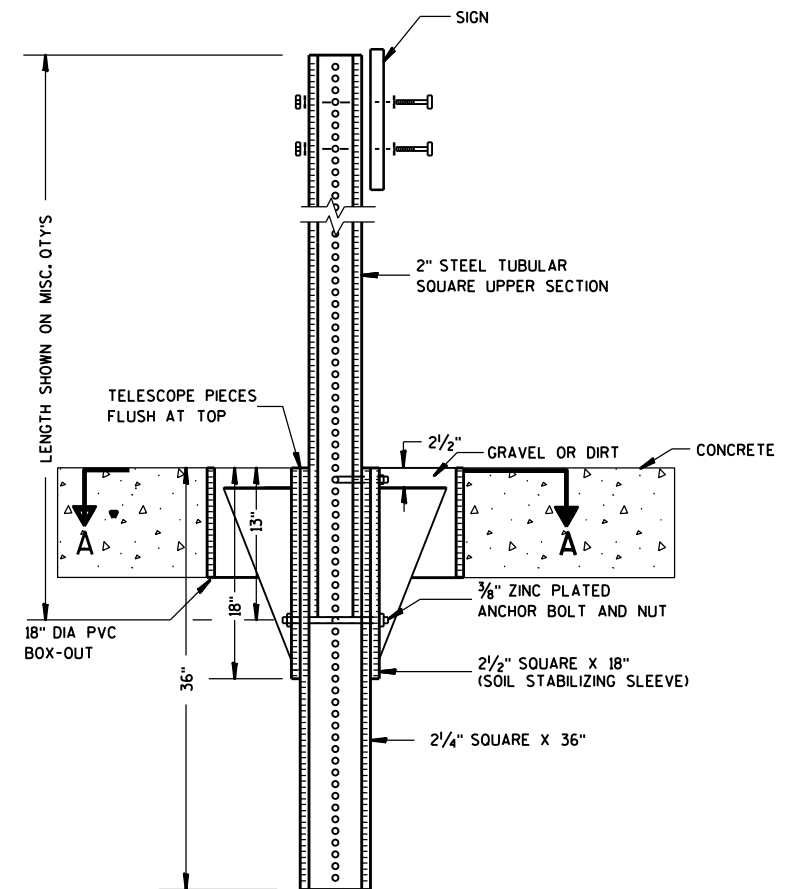
DATE 5/13/2020 PLATE NO. A4-3.22



ELEVATION VIEW

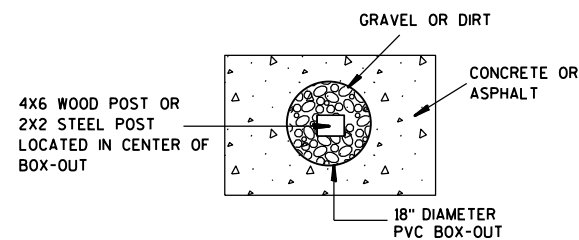
DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

- NOTES:**
1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
 2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
 3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



PLAN VIEW

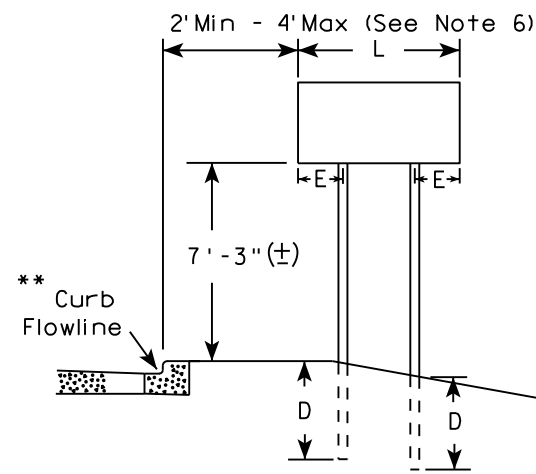
FOR NEW CONCRETE/ ASPHALT INSTALLATIONS

SIGN POST BOX-OUTS A4-3B	
<small>WISCONSIN DEPT OF TRANSPORTATION</small>	
APPROVED <i>Matthew R. Rauch</i> <small>for State Traffic Engineer</small>	
<small>DATE 1/27/14</small>	<small>PLATE NO. A4-3B.1</small>

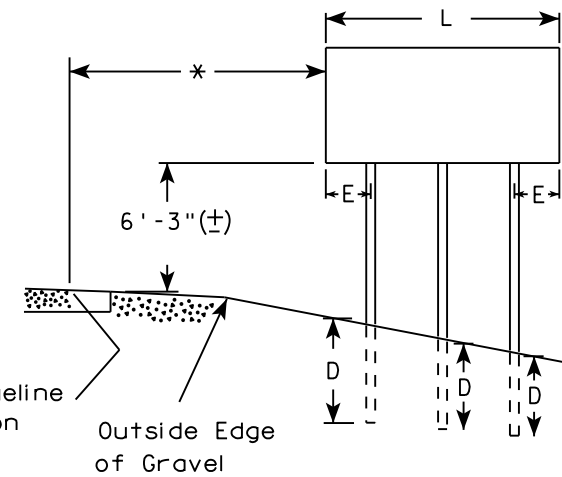
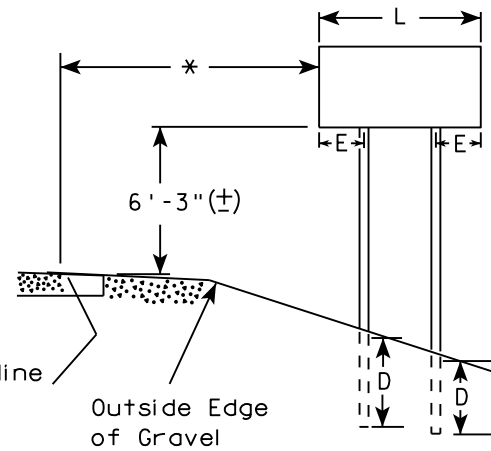
GENERAL NOTES

1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
2. See tables below for required number of posts.
3. For expressways and freeways, mounting height is 7'-3" (±) or 6'-3" (±) depending upon existence of sub-sign.
4. The (±) tolerance for mounting height is 3 inches.
5. J-Assemblies are considered to be one sign for mounting height.
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the engineer.
8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).

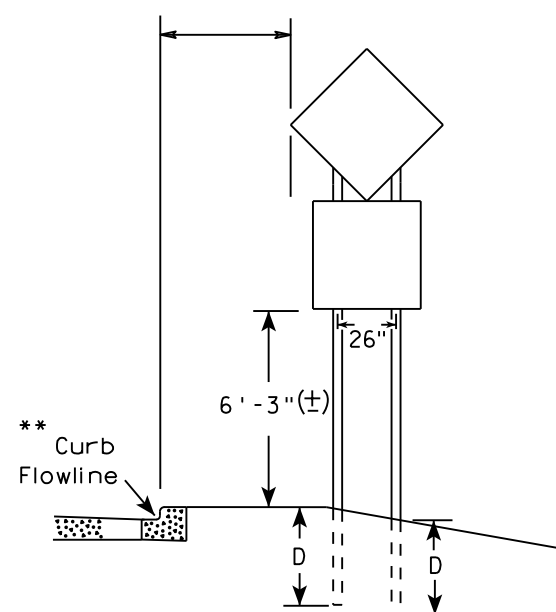
URBAN AREA



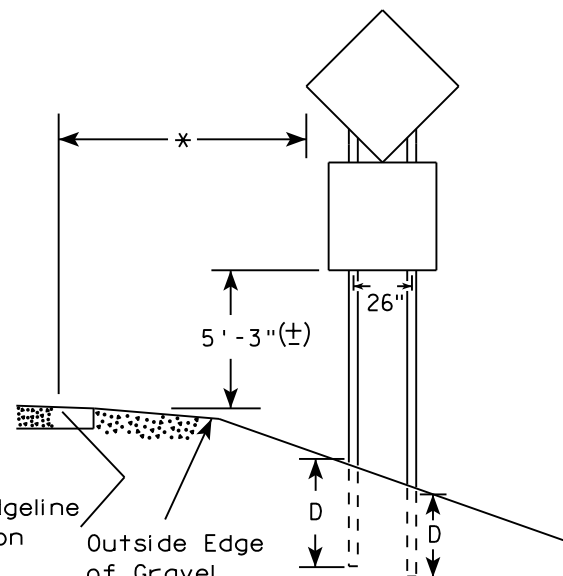
RURAL AREA (See Note 3)



2' Min - 4' Max (See Note 6)



48" DIAMOND WARNING SIGN



48" DIAMOND WARNING SIGN

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

*** See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)	
L	E
Greater than 48" Less than 60"	12"
60" to 108"	L/5

SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)	
L	E
Greater than 108" to 144"	12"

POST EMBEDMENT DEPTH

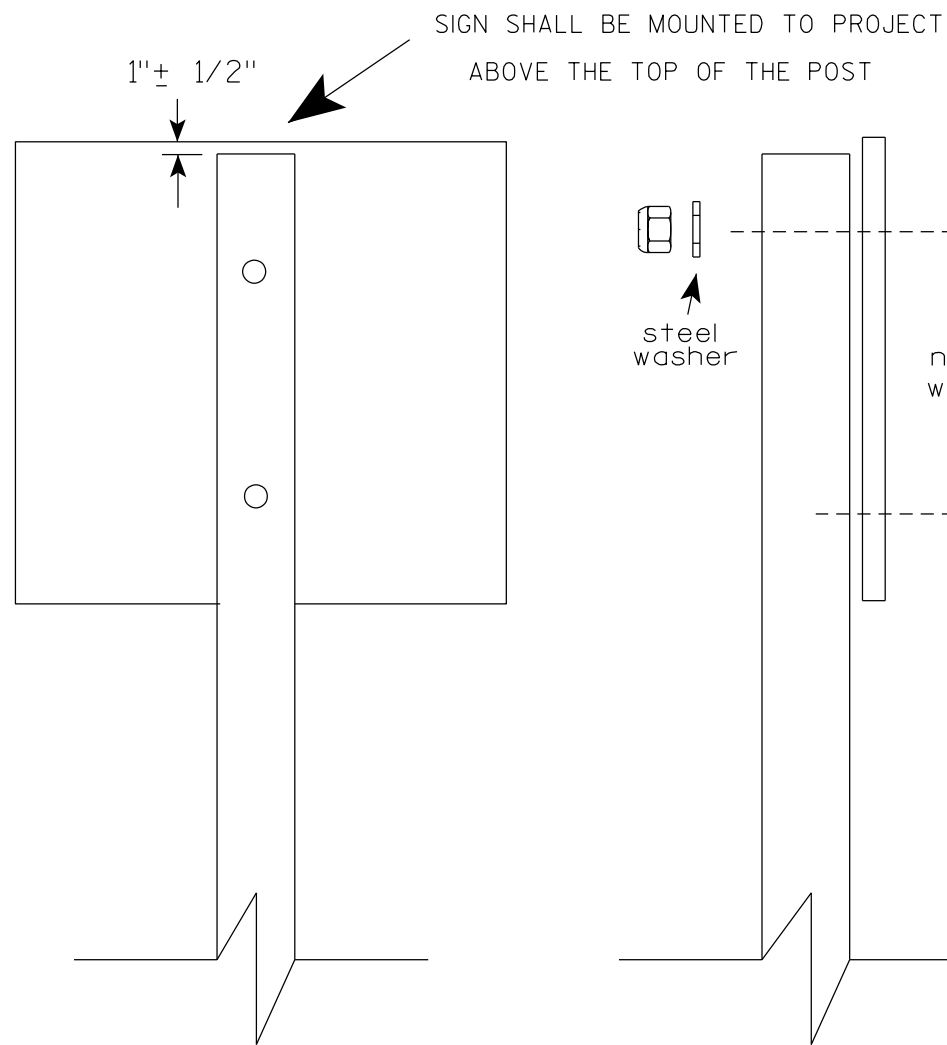
Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 8/21/17 PLATE NO. A4-4.15



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

- a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
- b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)

MACHINE BOLTS - 5/16" X 1-3/4" Length w/ lock nuts

WOOD POSTS (4" x 6")

LAG SCREWS - 3/8" X 3" (NO STRINGERS ON BACK OF SIGN)
3/8" X 4" (STRINGERS ON BACK OF SIGN)

SQUARE STEEL POSTS (2" x 2")

MACHINE BOLTS - 3/8" X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)
3/8" X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)

RIVETS - 9/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH

WASHERS (ALL POSTS) -

1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL

1-1/4" O.D. X 3/8" I.D. X .080 NYLON

* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS
TO POSTS

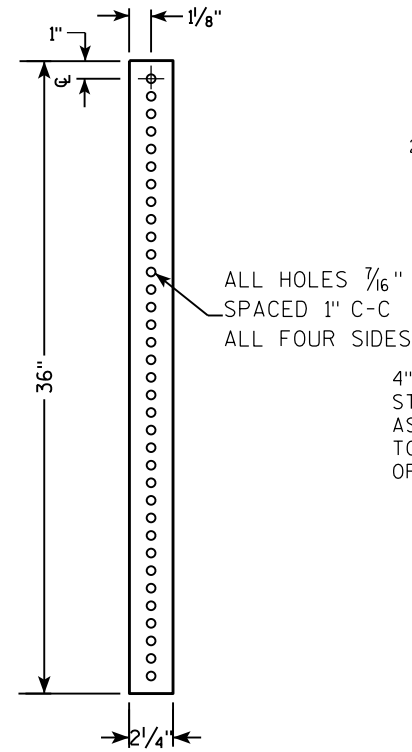
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Rauch*
For State Traffic Engineer

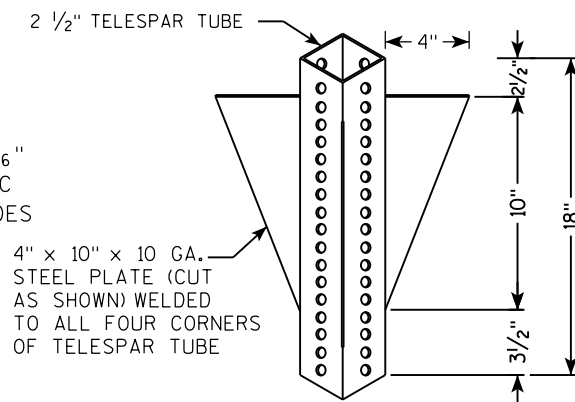
DATE 4/1/2020 PLATE NO. A4-8.9

**TELESCOPIC TUBING ANCHORS
TWO PIECE SYSTEM**

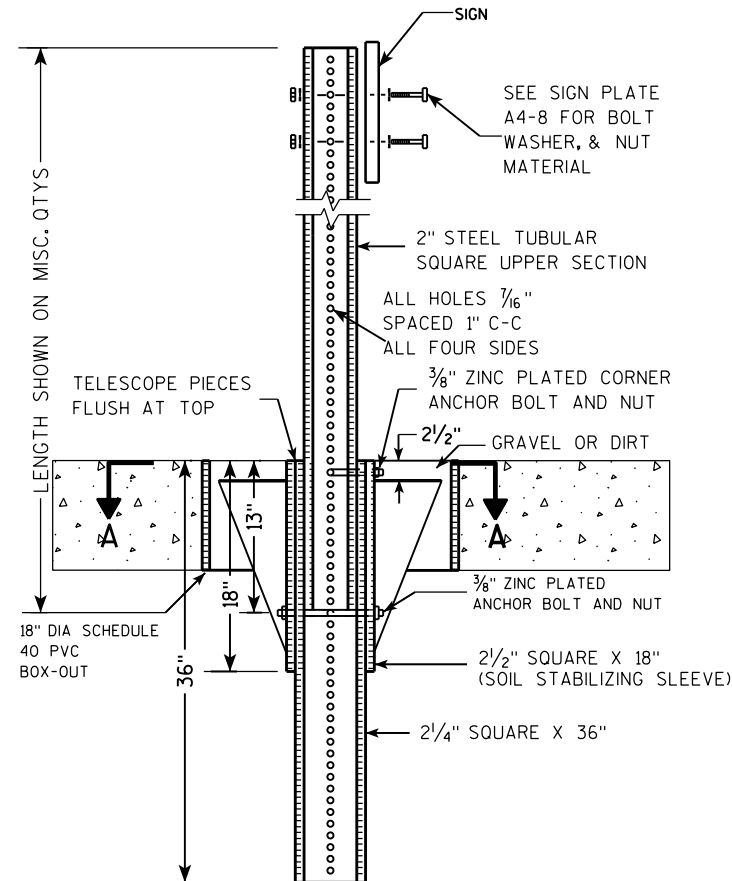
2 1/4" SQUARE
12 GAUGE
PERFORATED
GALVANIZED FINISH



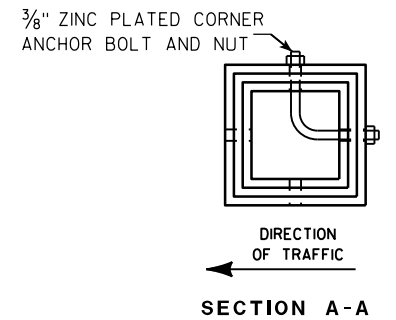
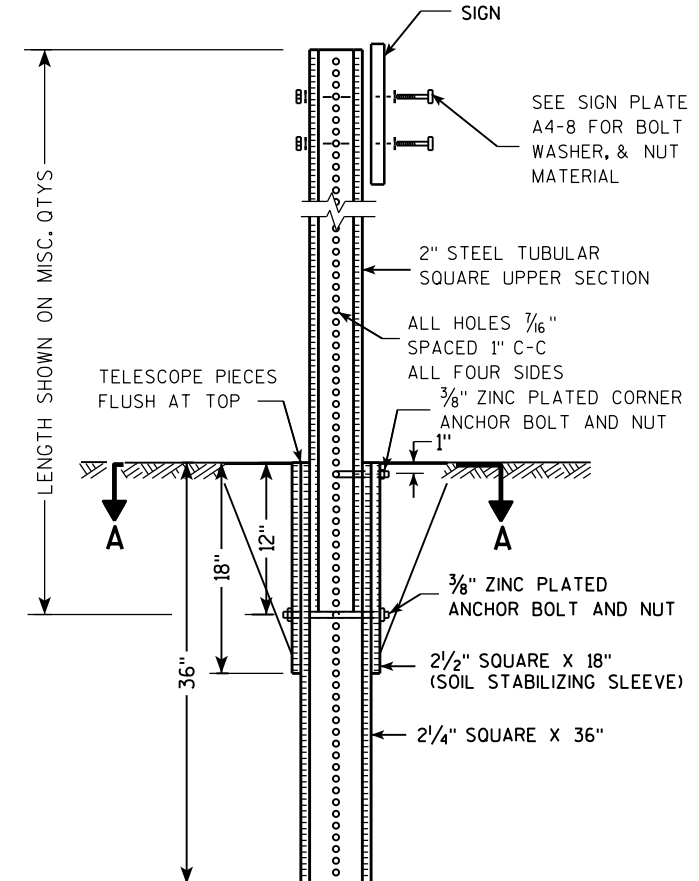
2 1/2" SQUARE
12 GAUGE
OMNI-DIRECTIONAL
PERFORATED
SOIL STABILIZING SLEEVE
GALVANIZED FINISH



**DETAIL OF TUBULAR STEEL SIGN POST
(IN POURED CONCRETE OR ASPHALT)**



**DETAIL OF TUBULAR STEEL SIGN POST
(IN LOCATIONS OTHER THAN POURED CONCRETE OR ASPHALT)**



Area of Sign Installation (Sq. Ft.)	Number of Required Posts
9 or less	1
Greater than 9 less than or equal to 18	2
Greater than 18 less than or equal to 27	3

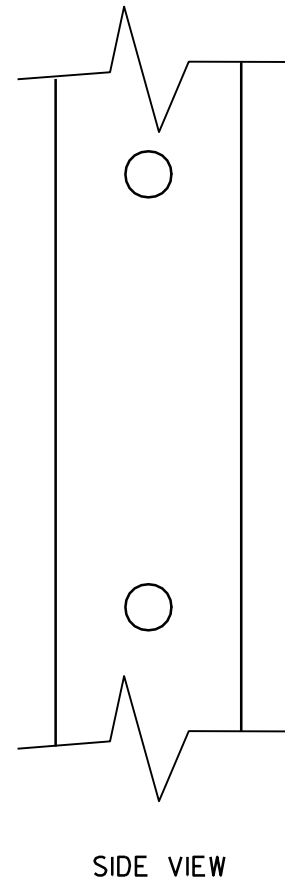
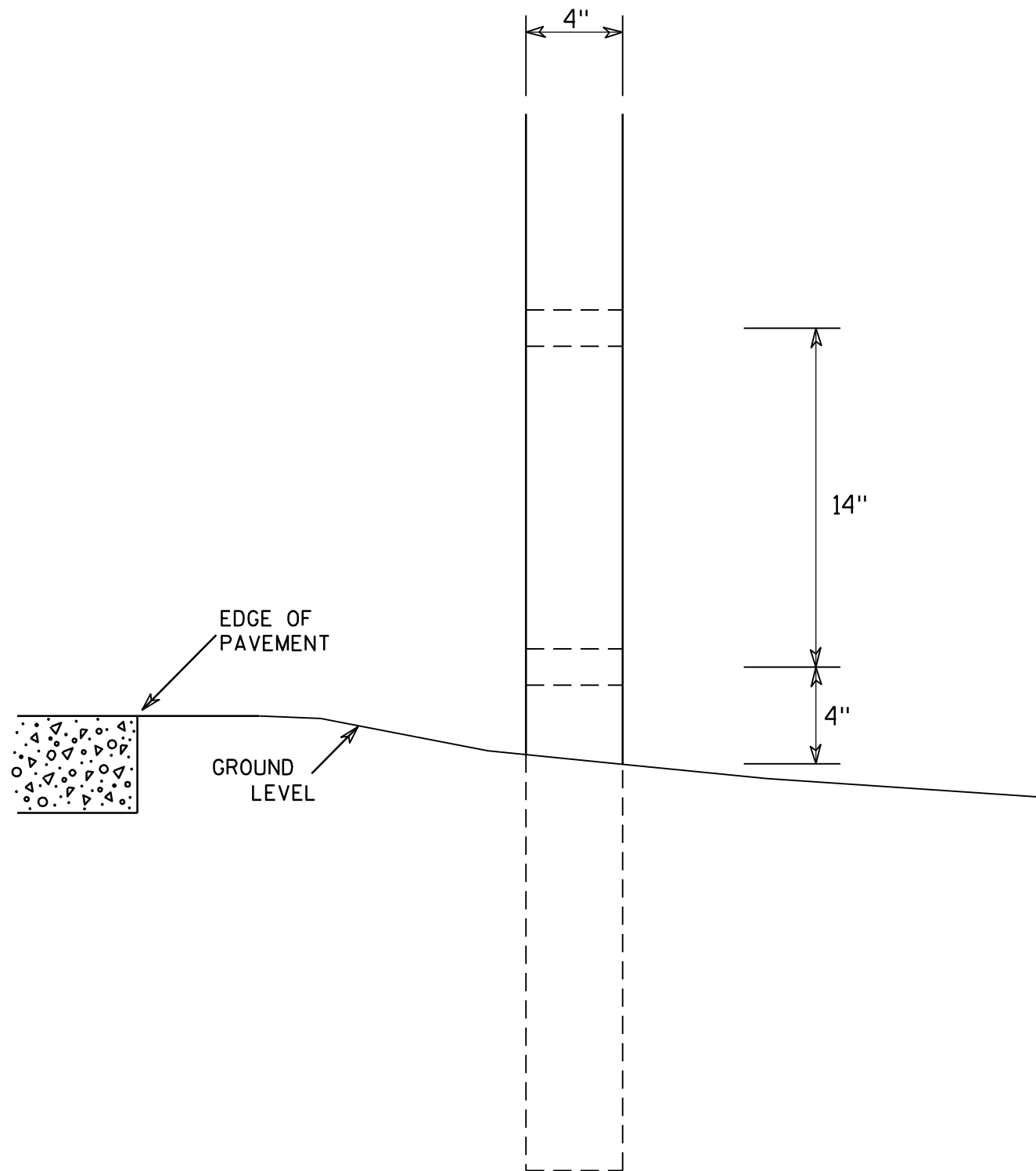
Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

**TUBULAR STEEL
SIGN POST
A4-9**

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 2/05/15 PLATE NO. A4-9.9



GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

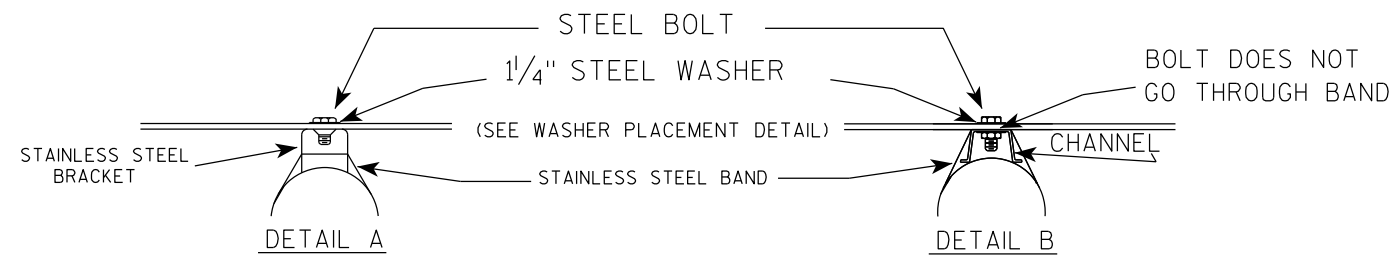
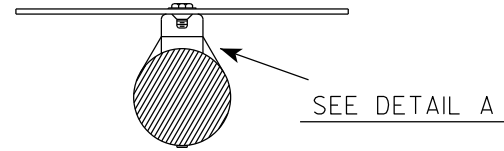
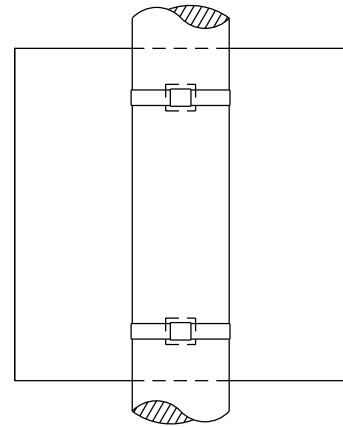
7

7

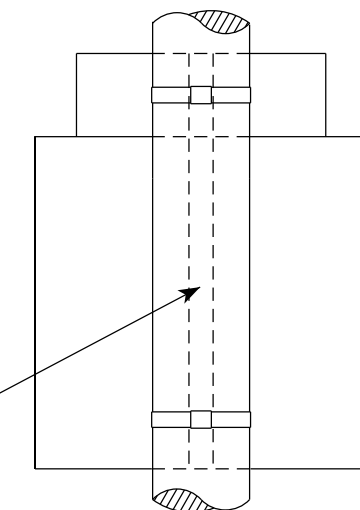
4 X 6 WOOD POST MODIFICATIONS	
<i>WISCONSIN DEPT OF TRANSPORTATION</i>	
APPROVED	<i>Chester J Spang</i> for State Traffic Engineer
DATE <u>3/27/97</u>	PLATE NO. <u>A4-11.2</u>

BANDING

SINGLE SIGN



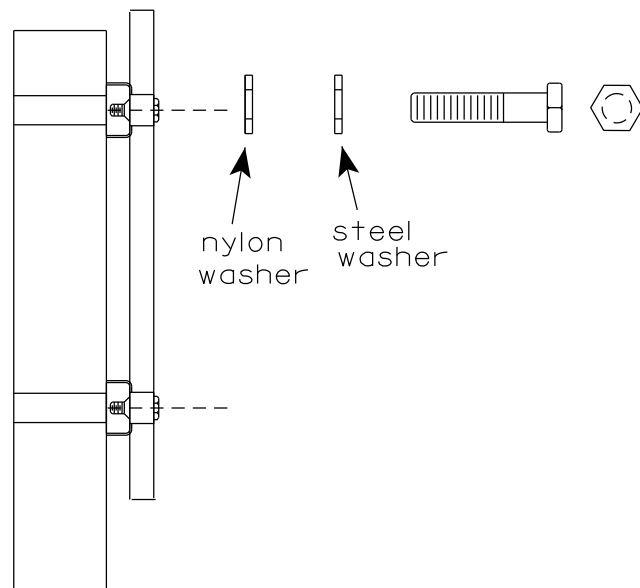
"J" ASSEMBLY



CHANNEL
SEE TYPICAL PANEL
INSTALLATION SHEET



WASHER PLACEMENT



WASHERS (ALL POSTS) -
1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
1-1/4" O.D. X 3/8" I.D. X .080 NYLON
FOR ALL TYPE H SIGNS

GENERAL NOTES

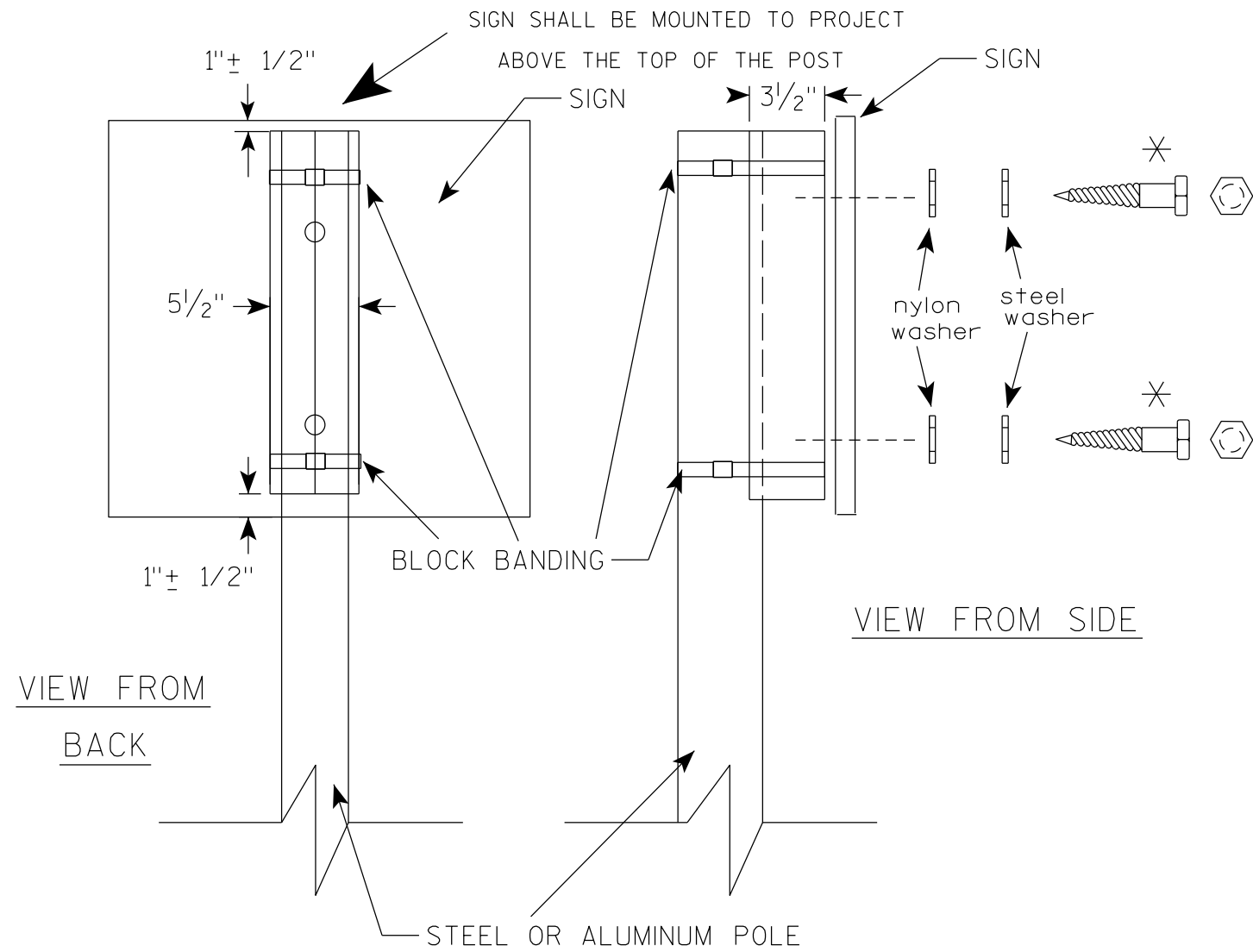
1. Any sign over 3 feet in width shall use the V-Block banding method. See A5-10 standard plate.
2. Signs 3 feet or greater in height shall have three bracket bands installed. Signs less than 3 feet in height shall have two bracket bands installed.
3. Banding and assembly bracket shall be stainless steel. All bands shall be 3/4" in width and 0.025" thickness.
4. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
 - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
 - b. Electro-galvanized in accordance with ASTM designation: B 633, Type III, SC 3

STANDARD SIGN
SIGN BANDING DETAILS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

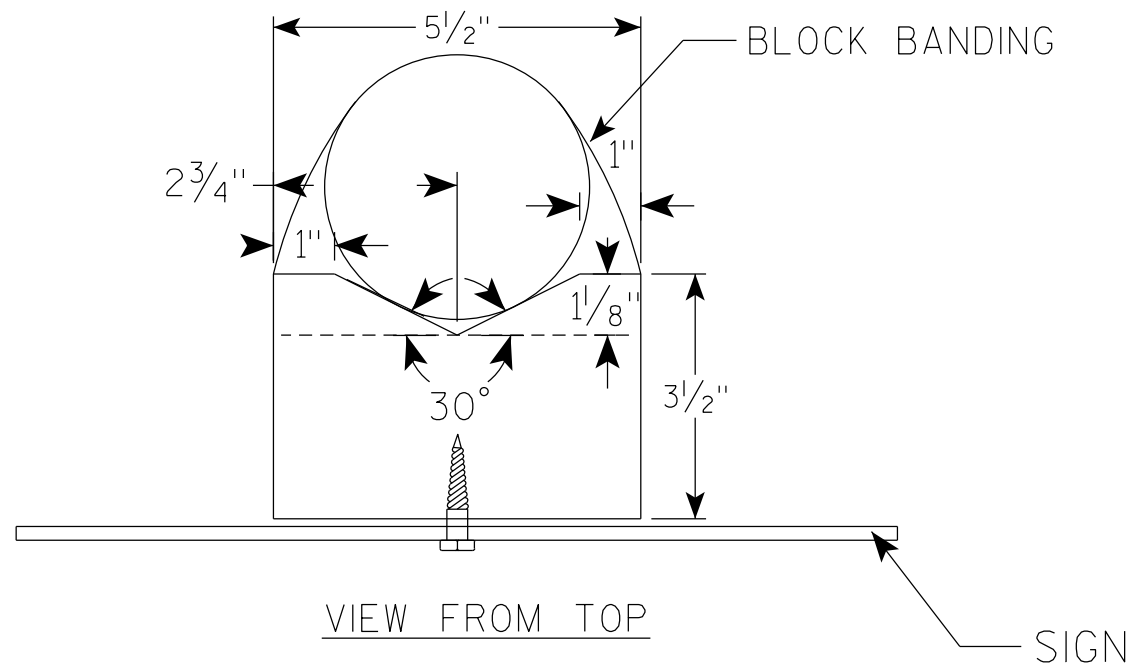
DATE 6/10/19 PLATE NO. A5-9.4



GENERAL NOTES

1. WOOD 4"X6" POST MATERIAL SHALL CONFORM TO 507.2.2 OF THE WISDOT STANDARD SPECIFICATIONS
2. BLOCK BANDING AND CLIPS SHALL BE STAINLESS STEEL, 3/4" WIDTH AND 0.025" THICKNESS
3. SIGNS 3' OR GREATER IN HEIGHT SHALL UTILIZE 3 BLOCK BANDS. SIGNS UNDER 3' IN HEIGHT SHALL UTILIZE 2 BLOCK BANDS
4. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA, BUT NORMALLY THERE ARE TWO. FOR SIGNS GREATER THAN 9 S.F. 3 FASTENERS SHALL BE USED.
5. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
 - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
 - b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3
6. ALL BOLTS SHALL HAVE HEXAGONAL HEADS.
7. STEEL WASHERS SHALL BE 1/4" O.D. X 3/8" I.D. X 1/16"
8. NYLON WASHERS SHALL BE 1/4" O.D. X 3/8" I.D. X .080 FOR TYPE H OR TYPE F FACE SIGN

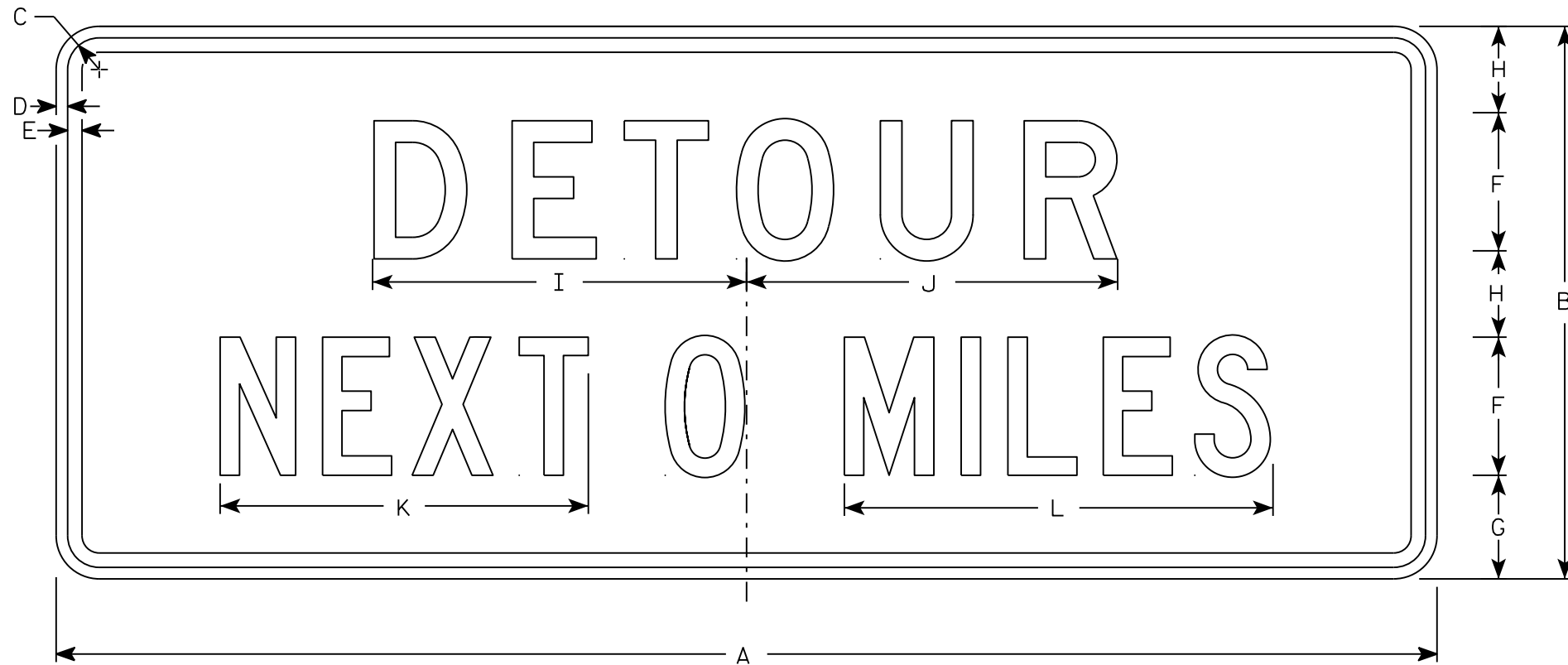
* LAG BOLTS SHALL BE 3/8" X 2 1/2"



BLOCK BANDING DETAIL (V-BLOCK OPTION)	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R. Rauch</i> for State Traffic Engineer
DATE 4/19/2022	PLATE NO. A5-10.3

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - Line 1 is D and Line 2 is C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Round distance to nearest whole Mile and substitute appropriate numerals and optically adjust spacing to achieve proper balance



G20-51

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	60	24	1 3/8	1/2	5/8	6	4 1/2	3 3/4	16 1/4	16 1/8	16	18 5/8															10
3																											
4	60	24	1 3/8	1/2	5/8	6	4 1/2	3 3/4	16 1/4	16 1/8	16	18 5/8															10
5																											

STANDARD SIGN
G20-51

WISCONSIN DEPT OF TRANSPORTATION

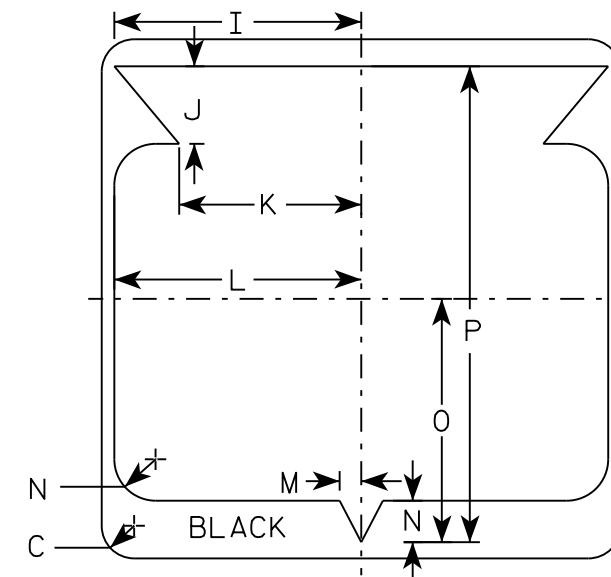
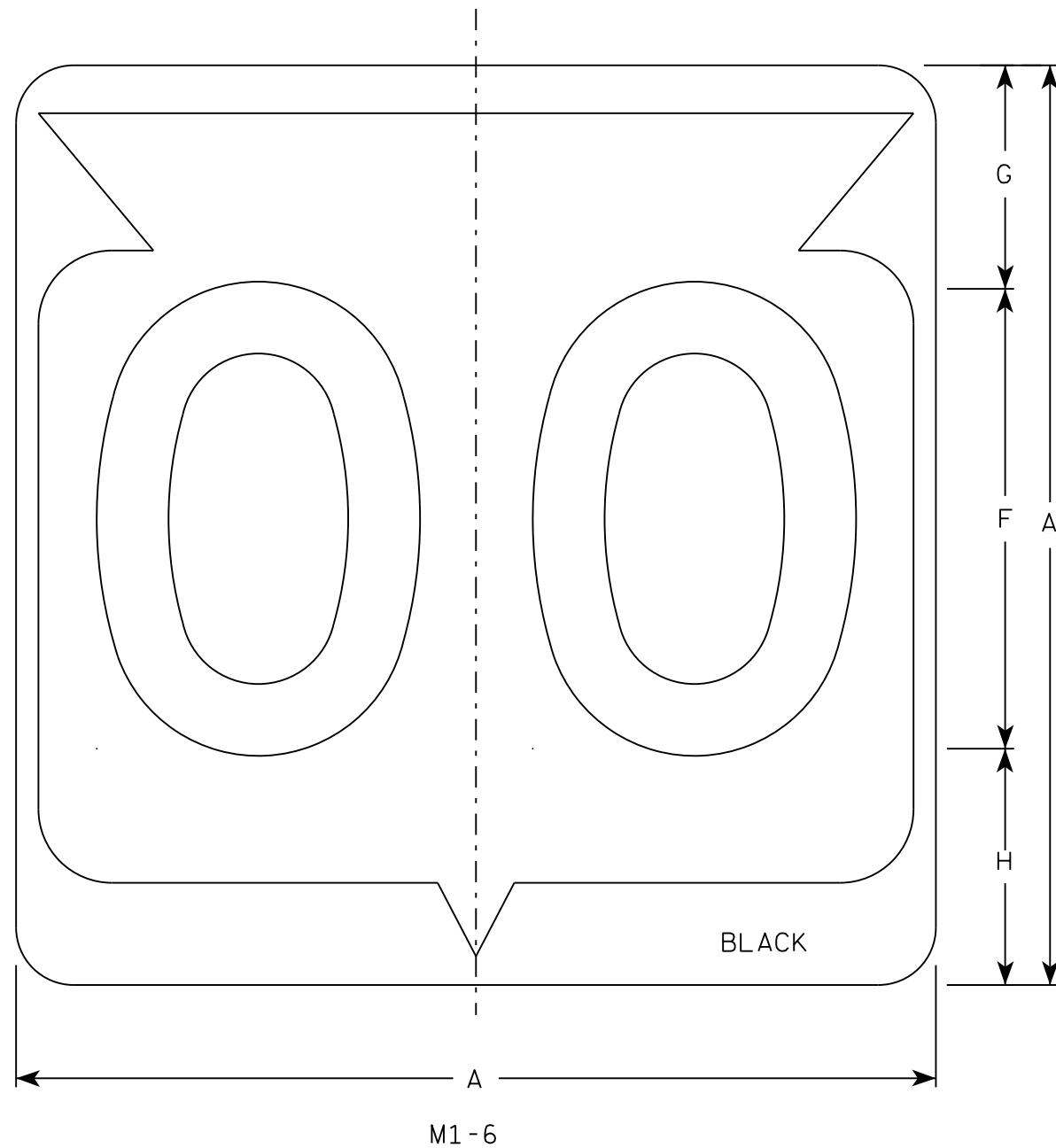
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 3/14/17 PLATE NO. G20-51.2

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - D except 3 number signs Series C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24		1 1/2			12	5 1/2	6 1/2	10 1/4	2 1/2	8 7/8	11 1/2	1	1 7/8	11 1/4	21 7/8											4.0
3	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0
4	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0
5	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0

STATE ROUTE MARKER
M1-6 FOR ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 3/16/18 PLATE NO. M1-6.10

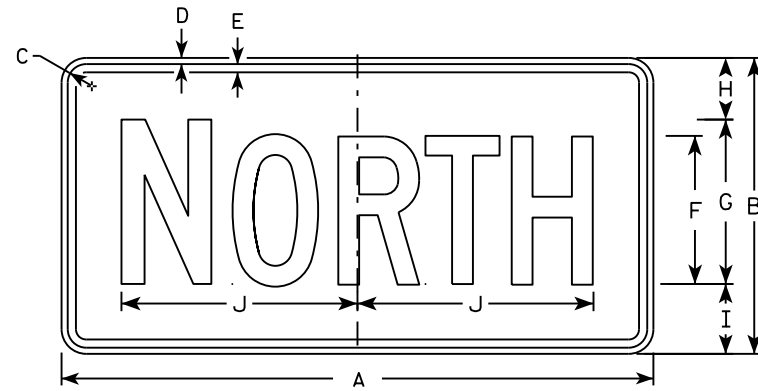
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

7

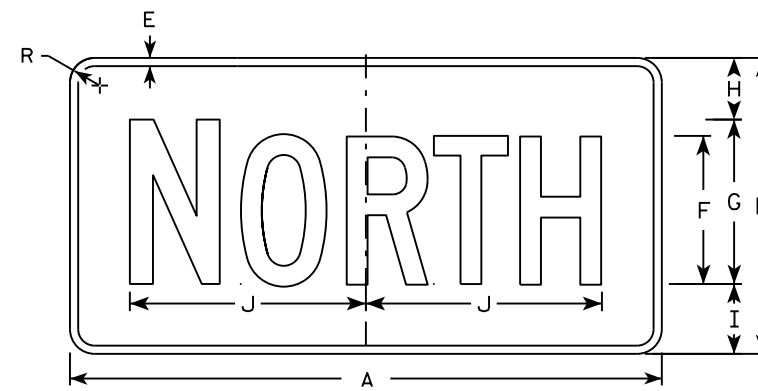
7

NOTES

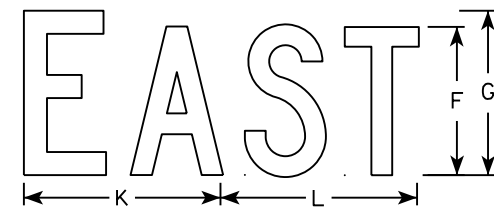
- All Signs Type II - Type H
- Color:
 - Background - See note 5
 - Message - See note 5
- Message Series - C
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- M3-1 thru M3-4 Background - White
 Message - Black
 MB3-1 thru MB3-4 Background - Blue
 Message - White
 MK3-1 thru MK3-4 Background - Green
 Message - White
 MM3-1 thru MM3-4 Background - White
 Message - Green
 MN3-1 thru MN3-4 Background - Brown
 Message - White
 MP3-1 thru MP3-4 Background - White
 Message - Blue
- Note the first letter of each direction is larger than the remainder of the message.



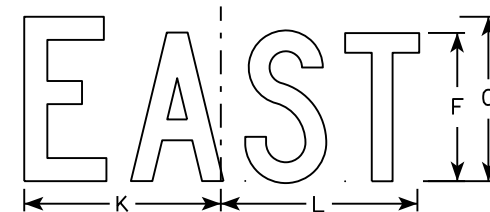
M3-1
MM3-1
MP3-1



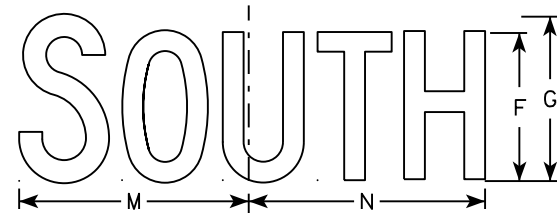
MB3-1
MK3-1
MN3-1



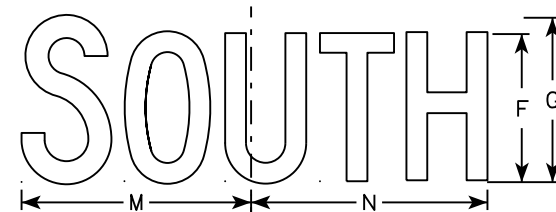
M3-2
MM3-2
MP3-2



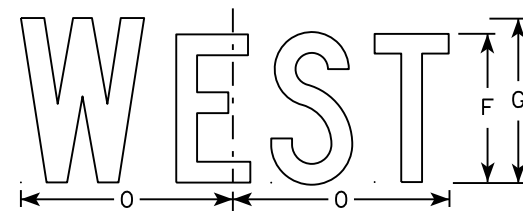
MB3-2
MK3-2
MN3-2



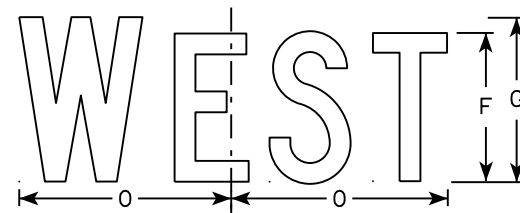
M3-3
MM3-3
MP3-3



MB3-3
MK3-3
MN3-3



M3-4
MM3-4
MP3-4



MB3-4
MK3-4
MN3-4

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	12	1 1/8	3/8	3/8	6	7	2 1/4	2 3/4	10 1/4	7 7/8	8 3/8	10 1/4	9 3/4	8 3/4			1 1/2									2.00
3	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
4	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
5	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5

STANDARD SIGNS
M3-1 thru M3-4
SERIES

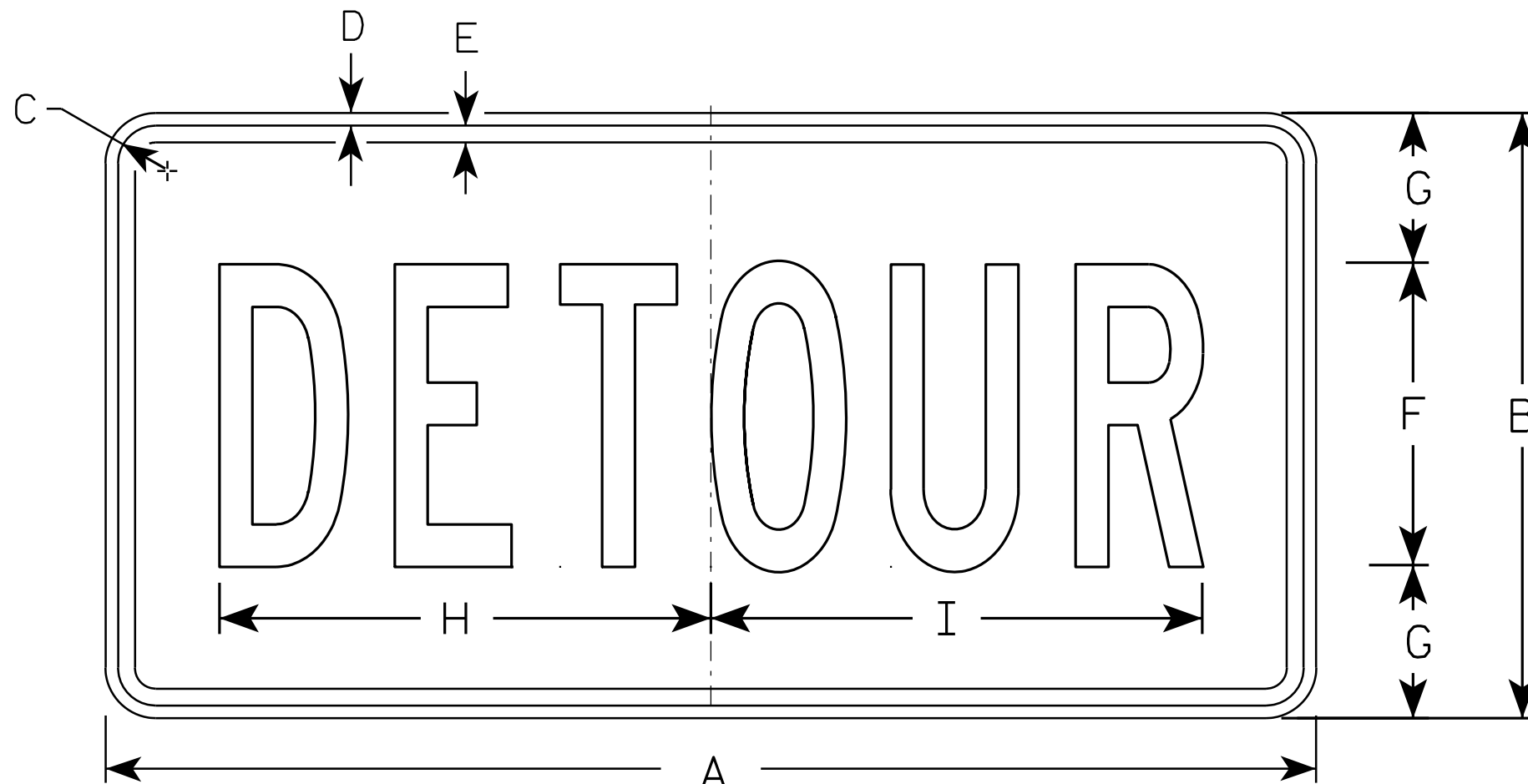
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 10/15/15 PLATE NO. M3-1.14

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - B
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



M4-8

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	12	1 1/8	3/8	3/8	6	3	10	10 1/4																		2.0
3	36	18	1 1/8	3/8	1/2	9	4 1/2	14 5/8	14 1/2																		4.5
4																											
5																											

STANDARD SIGN
M4-8

WISCONSIN DEPT OF TRANSPORTATION

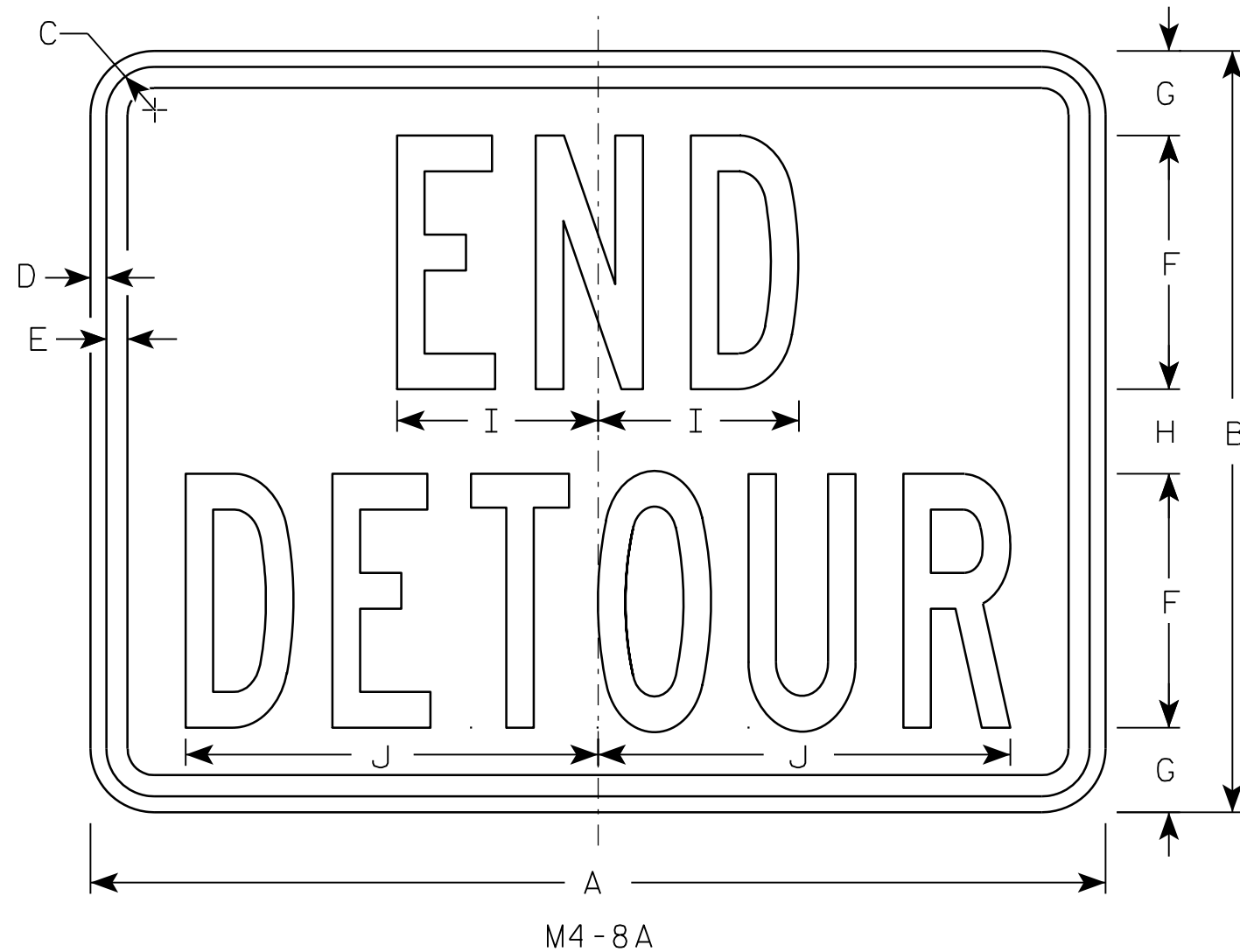
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 11/10/10 PLATE NO. M4-8.2

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - B
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	18	1 1/8	3/8	1/2	6	2	2	4 3/4	9 3/4																	3.0
3	30	24	1 1/8	3/8	1/2	8	2 1/2	3	6 3/4	13																	5.0
4																											
5																											

STANDARD SIGN
M4-8A

WISCONSIN DEPT OF TRANSPORTATION

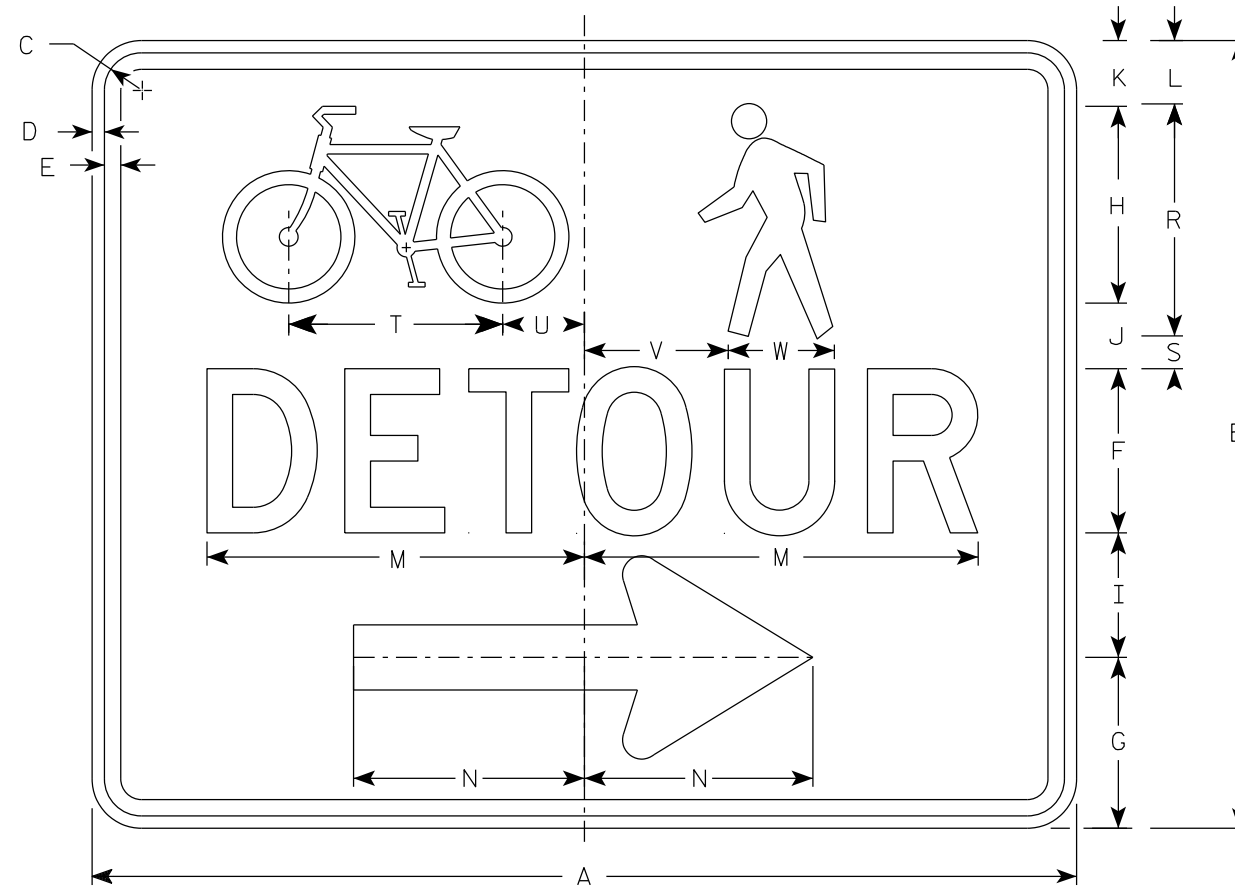
APPROVED *Matthew R Rauch*
for State Traffic Engineer

DATE 3/9/11 PLATE NO. M4-8A.2

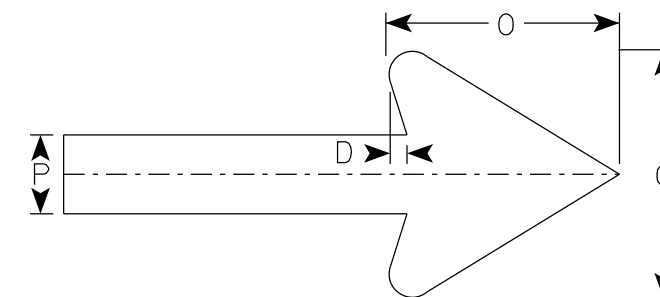
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II-Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. M4-9AL is the same as M4-9AR except the arrow is reversed.



M4-9AR



Arrow Detail

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	30	24	1 1/8	3/8	1/2	5	5 1/4	6	3 3/4	2	2	1 7/8	11 3/4	7	6	2		7 1/8	1	6 1/2	2 1/2	4 3/8	3 1/4				5.00
3																											
4																											
5																											

STANDARD SIGN
M4-9A L&R

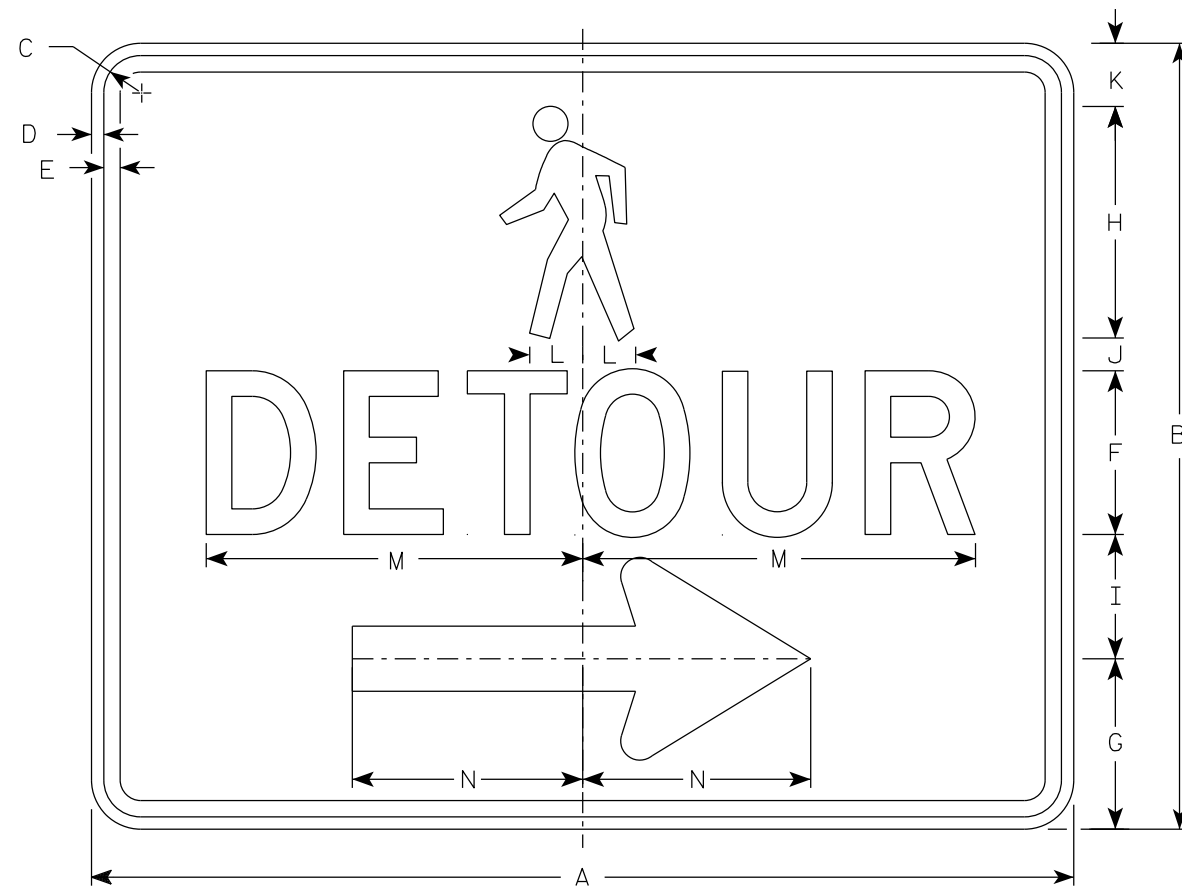
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Rauch*
For State Traffic Engineer

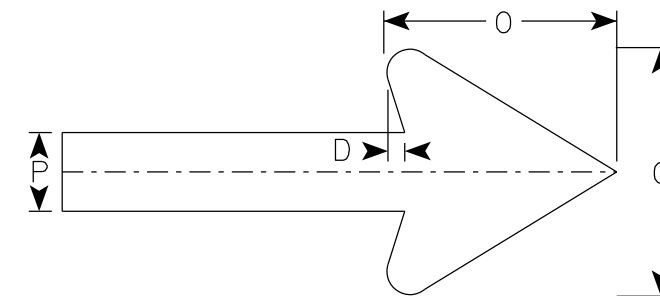
DATE 7/2/19 PLATE NO. M4-9A.1

NOTES

1. Sign is Type II-Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. M4-9BL is the same as M4-9BR except the arrow is reversed.



M4 - 9BR



Arrow Detail

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	30	24	1 1/8	3/8	1/2	5	5 1/4	7 1/8	3 3/4	1	1 1/8	1 5/8	11 3/4	7	6	2											5.00
3																											
4																											
5																											

STANDARD SIGN
M4-9B L&R

WISCONSIN DEPT OF TRANSPORTATION

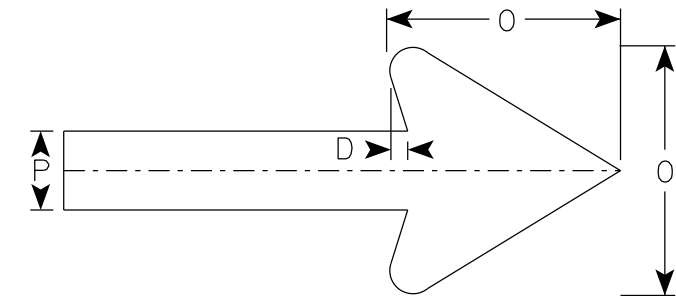
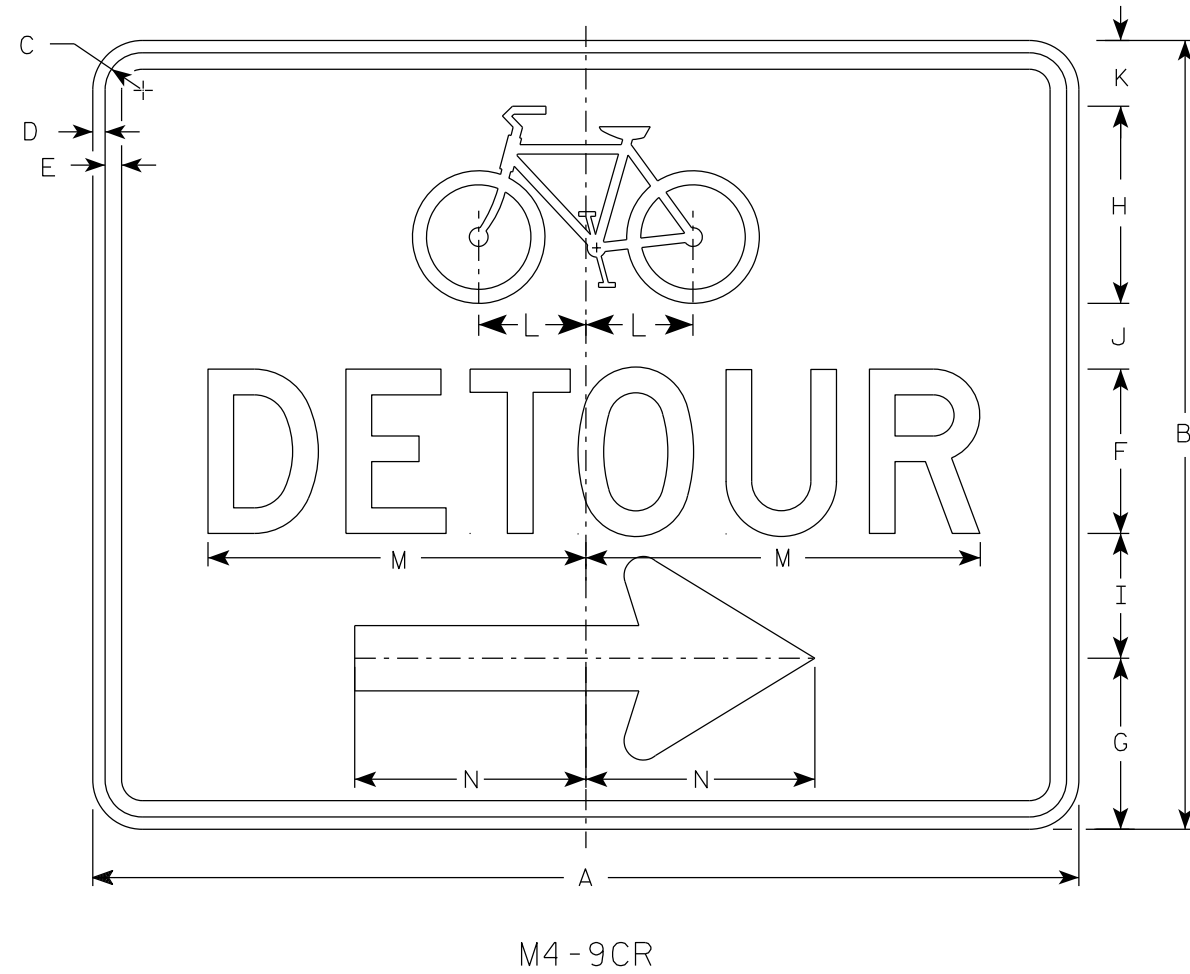
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 7/1/19 PLATE NO. M4-9B.2

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II-Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. M4-9CL is the same as M4-9CR except the arrow is reversed.



Arrow Detail

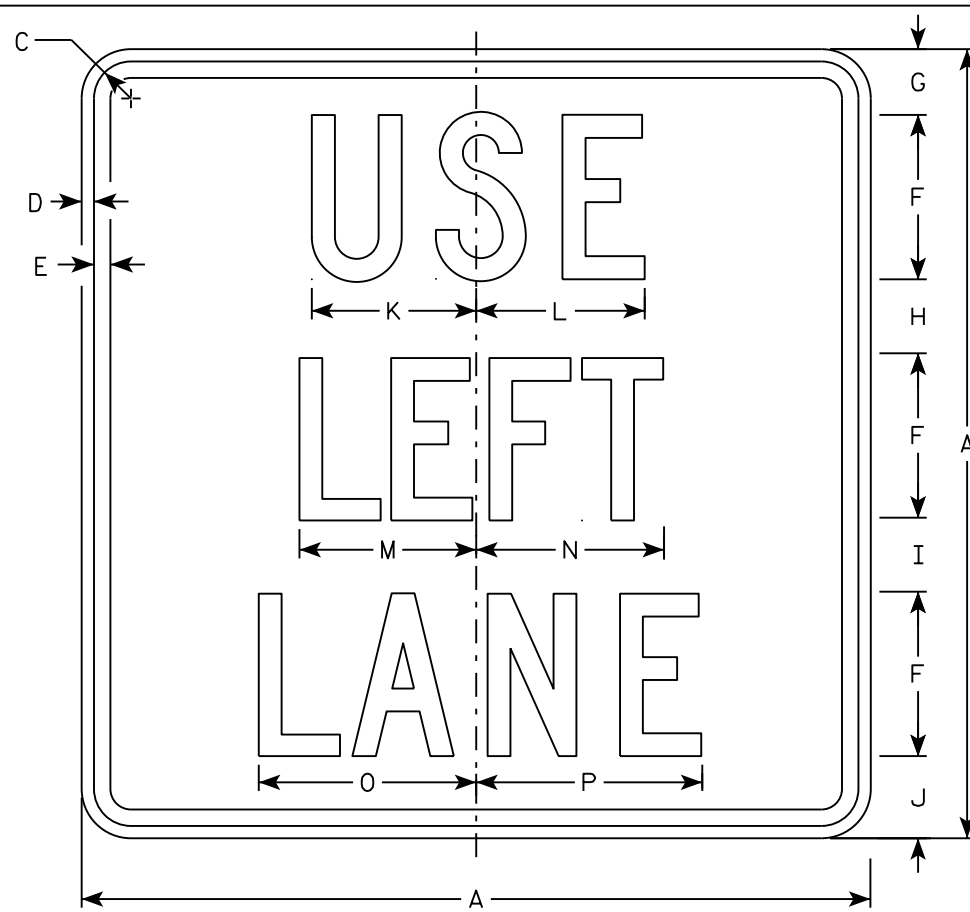
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	30	24	1 1/8	3/8	1/2	5	5 1/4	6	3 3/4	2	2	3 1/4	11 3/4	7	6	2											5.00
3																											
4																											
5																											

STANDARD SIGN
M4-9C L&R

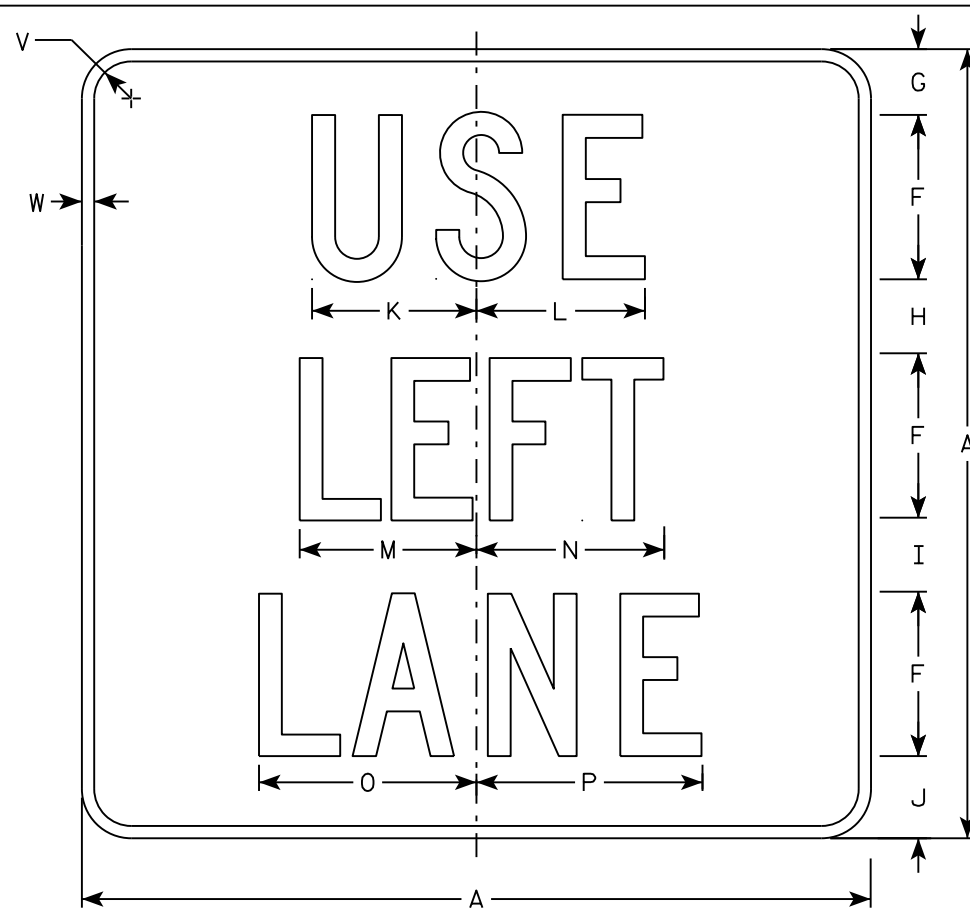
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Rauch*
For State Traffic Engineer

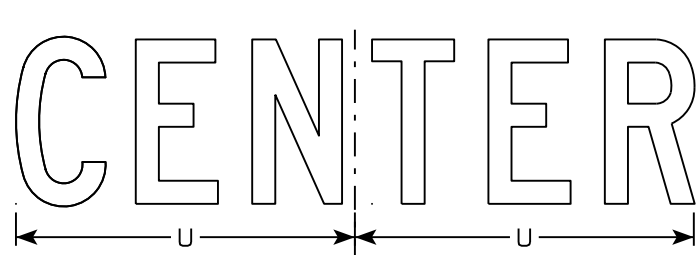
DATE 7/2/19 PLATE NO. M4-9C.1



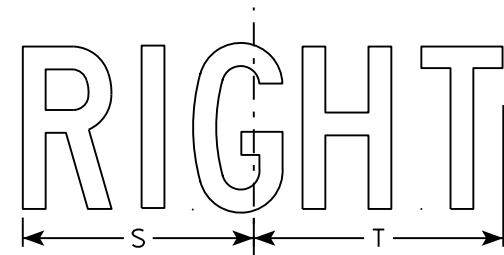
M4-20L
MM4-20L
M04-20L
MP4-20L



MB4-20L
MK4-20L
MN4-20L
MR4-20L



M4-20C
MB4-20C
MK4-20C
MM4-20C
MN4-20C
M04-20C
MP4-20C
MR4-20C



M4-20R
MB4-20R
MK4-20R
MM4-20R
MN4-20R
M04-20R
MP4-20R
MR4-20R

NOTES

1. Sign is Type II - Type H except as Shown
2. Color:
Background - See note 5
Message - See note 5
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. M4-20 Background - White
Message - Black
MB4-20 Background - Blue
Message - White
MK4-20 Background - Green
Message - White
MM4-20 Background - White
Message - Green
MN4-20 Background - Brown
Message - White
M04-20 Background - Orange - Type F Reflective
Message - Black
MP4-20 Background - White
Message - Blue
MR4-20 Background - Brown
Message - Yellow

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24		1 1/8	3/8	1/2	5	2	2 1/4	2 1/4	2 1/2	5	5 1/8	5 3/8	5 3/4	6 5/8	6 7/8			7	7 5/8	10 1/4	1 1/2	1/2				4.0
3	36		1 5/8	5/8	3/4	7	4	3	3 1/2	4 1/2	7 1/2	7 3/4	8	8 5/8	9 7/8	10 1/4			10 3/8	11 3/8	14 3/8	1 7/8	1/2				9.0
4																											
5																											

STANDARD SIGN
M4-20

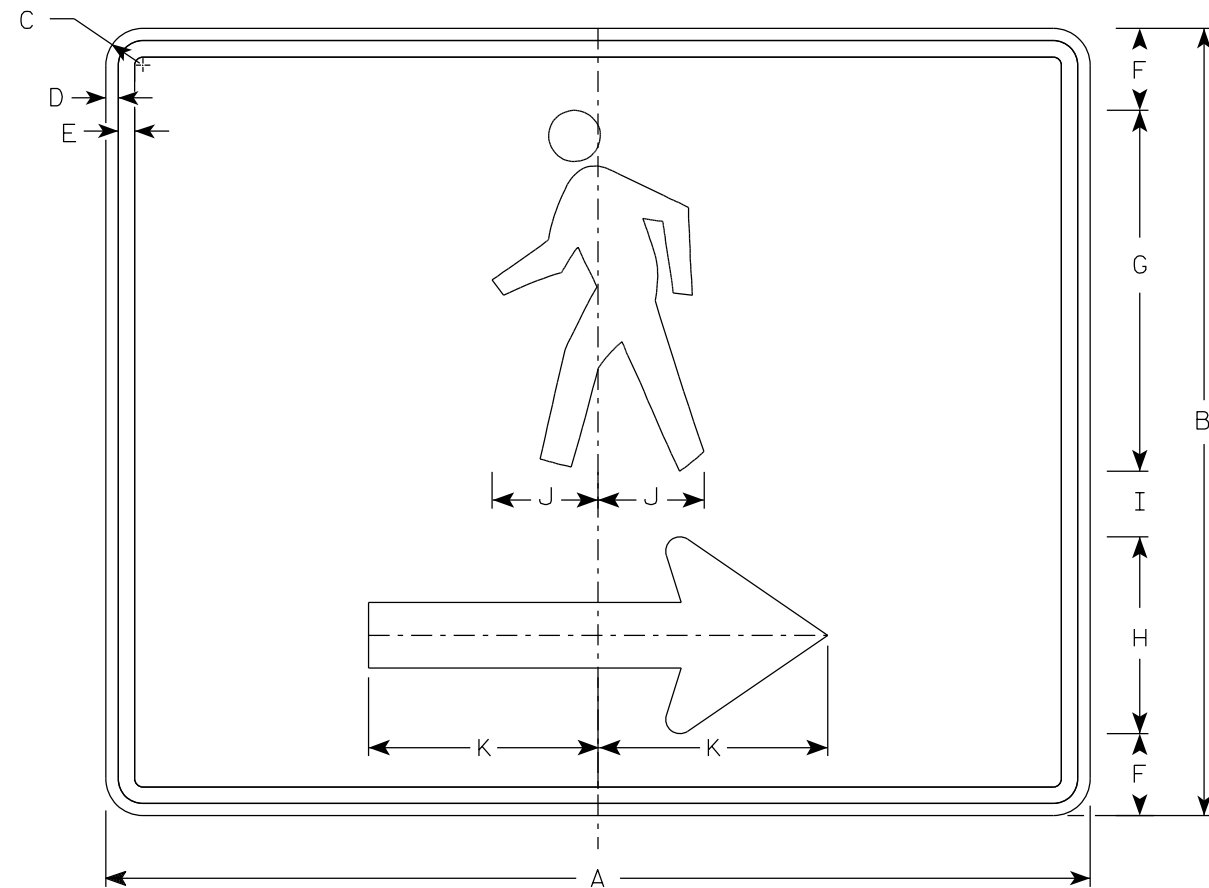
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Rauch*
For State Traffic Engineer

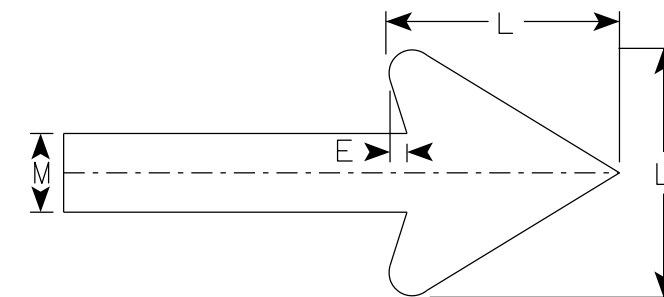
DATE 10/15/15 PLATE NO. M4-20.5

NOTES

1. Sign is Type II- Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. M4-60L is the same as M4-60R except the arrow is reversed.



M4-60R



Arrow Detail

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	30	24	1 1/8	3/8	1/2	2 1/2	11	6	2	3 1/4	7	6	2														5.00
3																											
4																											
5																											

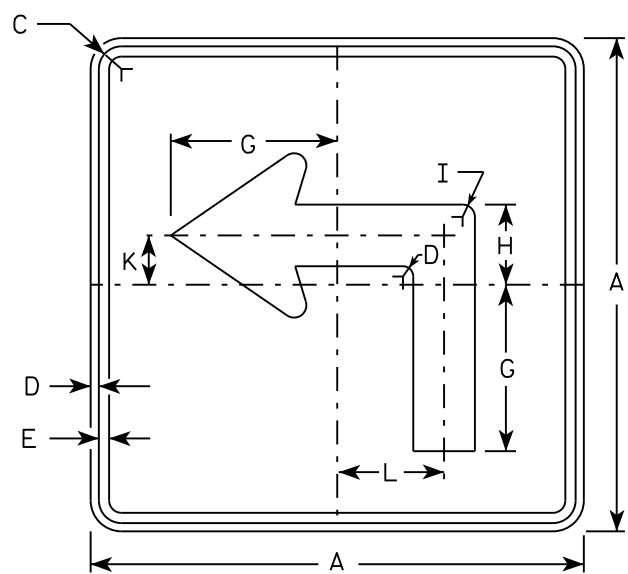
STANDARD SIGN
M4-60 L&R

WISCONSIN DEPT OF TRANSPORTATION

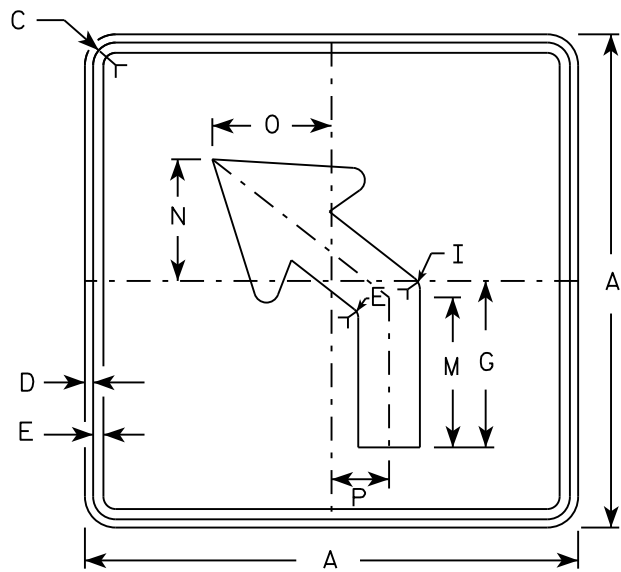
APPROVED *Matthew R Rauch*
For State Traffic Engineer

DATE 9/16/2021 PLATE NO. M4-60.1

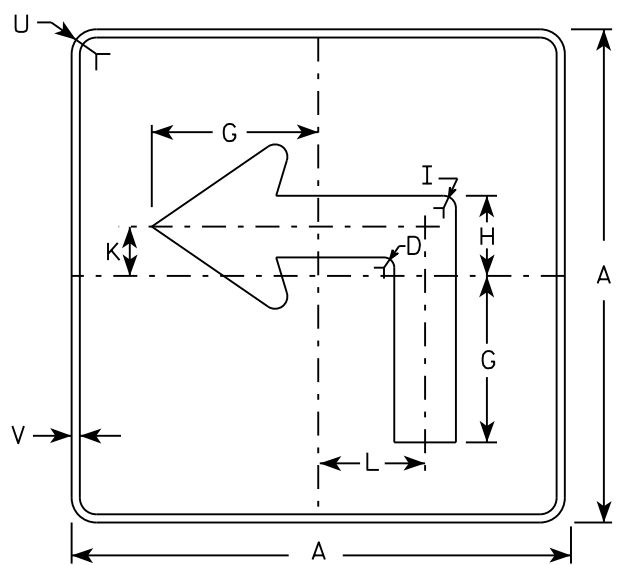
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**



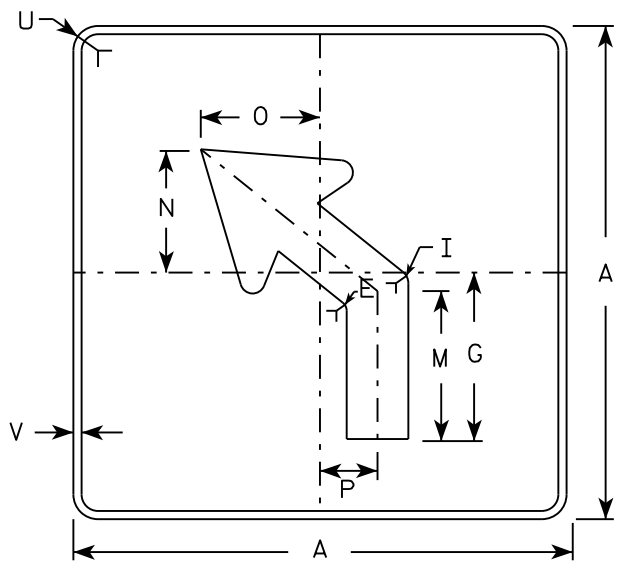
M5-1L
MM5-1L
M05-1L
MP5-1L



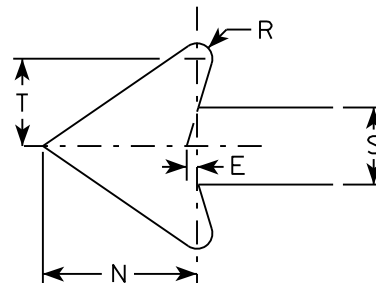
M5-2L
MM5-2L
M05-2L
MP5-2L



MB5-1L
MK5-1L
MN5-1L
MR5-1L



MB5-2L
MK5-2L
MN5-2L
MR5-2L



NOTES

- Signs are Type II - Type H reflective except as shown
- Color:
 - Background - See note 4
 - Message - See note 4
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- M5-1 and M5-2 Background - White
Message - Black
 - MB5-1 and MB5-2 Background - Blue
Message - White
 - MK5-1 and MK5-2 Background - Green
Message - White
 - MM5-1 and MM5-2 Background - White
Message - Green
 - MN5-1 and MN5-2 Background - Brown
Message - White
 - M05-1 and M05-2 Background - Orange - Type F Reflective
Message - Black
 - MP5-1 and MP5-2 Background - White - Type H Reflective
Message - Blue
 - MR5-1 and MR5-2 Background - Brown
Message - Yellow
- M5-1R same as M5-1L except arrow points right.
- M5-2R same as M5-2L except arrow tilts right.

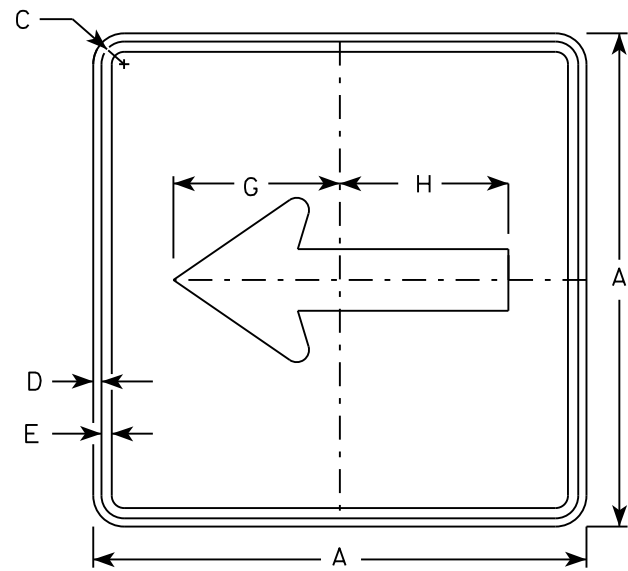
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7	3 3/8	5/8		2 1/8	4 1/2	6 3/8	5 1/4	5	2 1/2		1/2	2 5/8	3	1 1/2	1/2					3.06
3	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25
4	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25
5	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25

STANDARD SIGN
M5-1 & M5-2

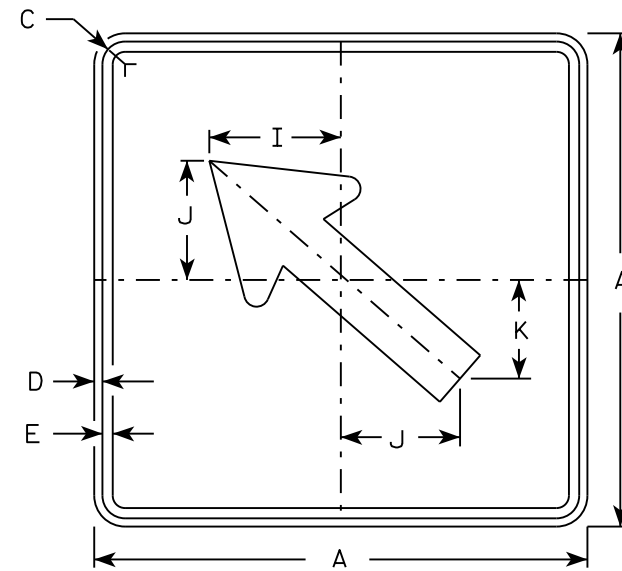
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

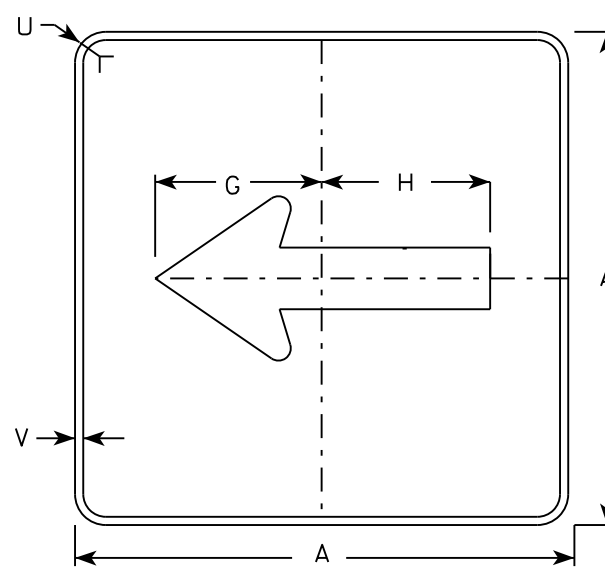
DATE 10/15/15 PLATE NO. M5-1.13



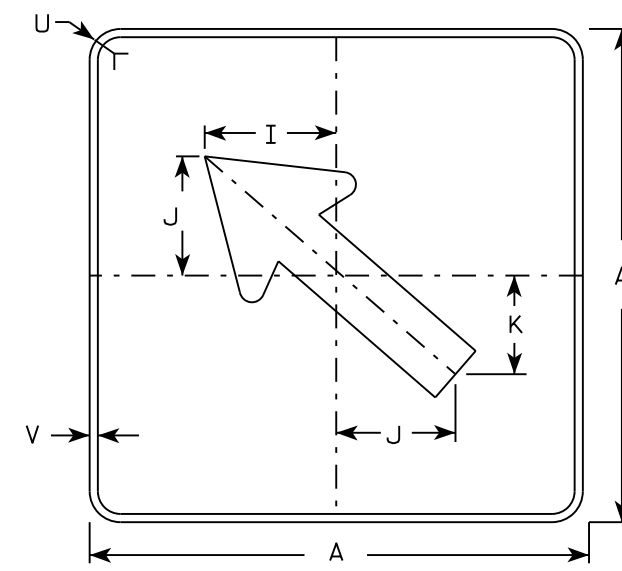
M6-1
MM6-1
M06-1
MP6-1



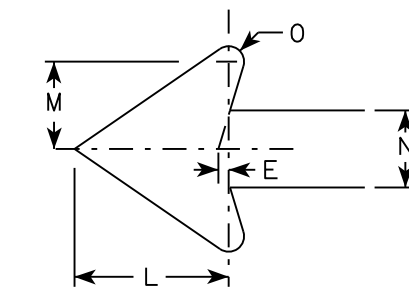
M6-2
MM6-2
M06-2
MP6-2



MB6-1
MK6-1
MN6-1
MR6-1



MB6-2
MK6-2
MN6-2
MR6-2



NOTES

- Signs are Type II - Type H except as Shown
- Color:
Background - See note 4
Message - See note 4
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- M6-1 and M6-2 Background - White
Message - Black
MB6-1 and MB6-2 Background - Blue
Message - White
MK6-1 and MK6-2 Background - Green
Message - White
MM6-1 and MM6-2 Background - White
Message - Green
MN6-1 and MN6-2 Background - Brown
Message - White
M06-1 and M06-2 Background - Orange - Type F Reflective
Message - Black
MP6-1 and MP6-2 Background - White
Message - Blue
MR6-1 and MR6-2 Background - Brown
Message - Yellow

7

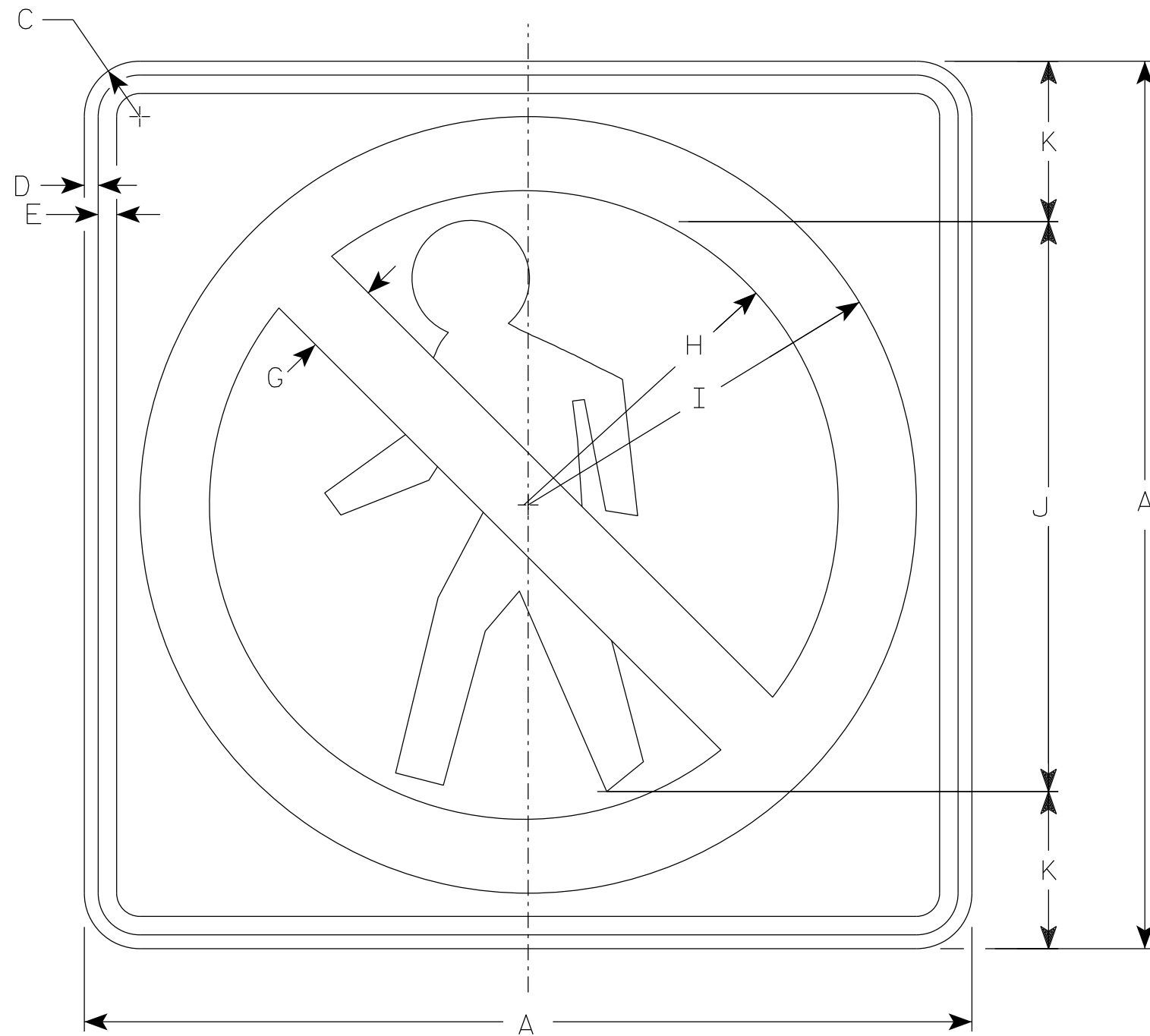
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7 1/2	7 1/8	5 5/8	5	4 1/4	5 1/4	3	2 5/8	1/2						1 1/2	1/2					3.06
3	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25
4	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25
5	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25

STANDARD SIGN
M6-1 & M6-2
SERIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 10/15/15 PLATE NO. M6-1.15



R9-3A

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - See note 3
3. Border & Pedestrian are non-reflective black.
Circle with diagonal bar is reflective red.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18		1 1/8	3/8	3/8		1 1/2	6 1/4	7 7/8	12	3																2.25
2S	24		1 1/8	3/8	1/2		2	8 1/2	10 1/2	15	4																4.00
2M	24		1 1/8	3/8	1/2		2	8 1/2	10 1/2	15	4																4.00
3																											
4																											
5																											

STANDARD SIGN
R9-3A

WISCONSIN DEPT OF TRANSPORTATION

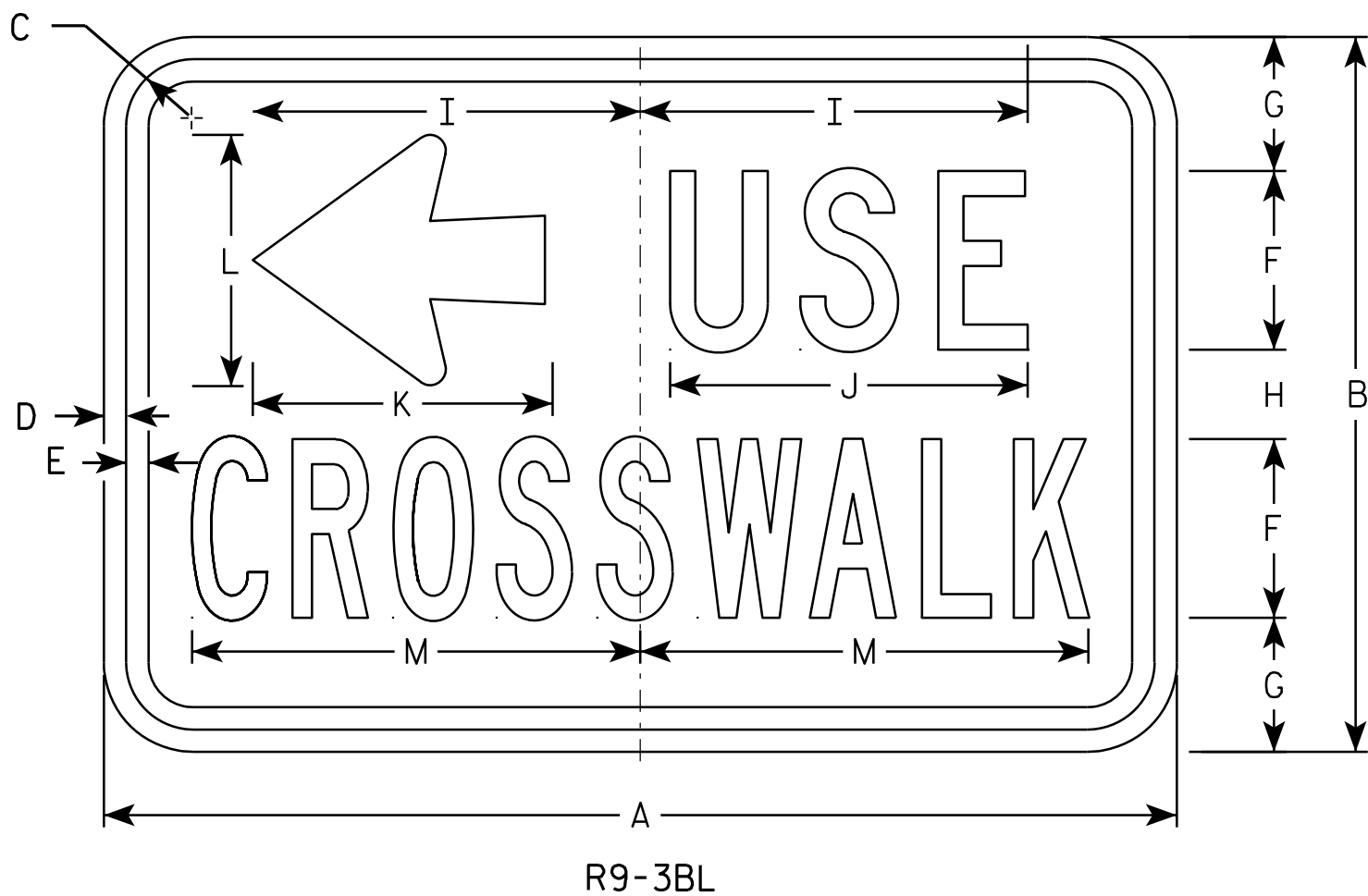
APPROVED *Matthew R Rauch*
for State Traffic Engineer

DATE 12/22/21 PLATE NO. R9-3A.3

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - Black
3. Message Series - C Line 1, Series B Line 2
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



7

Metric equivalent for this sign is:

SIZE	
1	
2	450 mm X 300 mm
3	
4	
5	

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.	Area m ²
1																												
2	18	12	1 1/8	3/8	3/8	3	2 1/4	1 1/2	6 1/2	6	5	4 1/4	7 1/2														1.5	0.14
3																												
4																												
5																												

STANDARD SIGN
R9-3BL

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

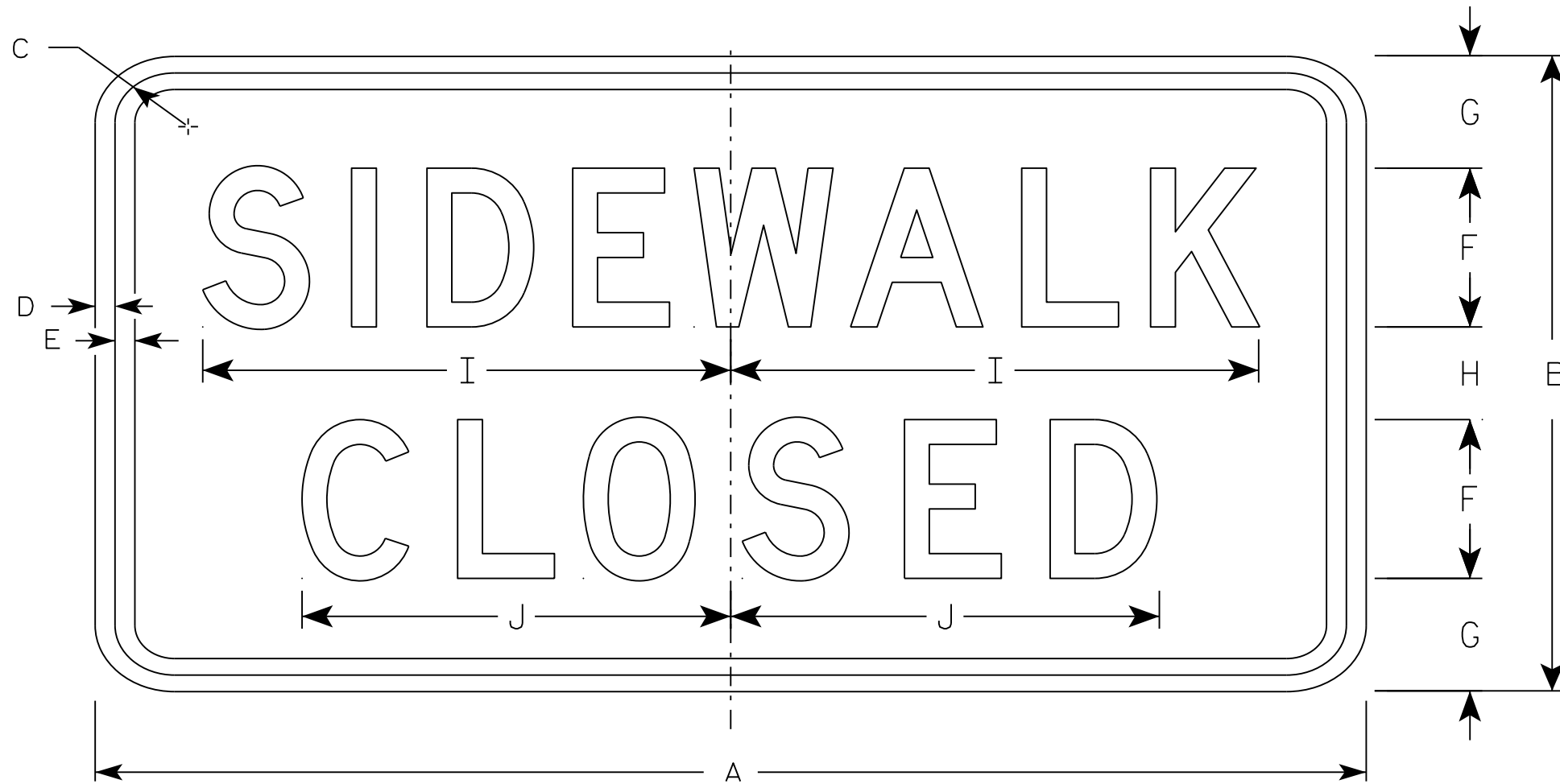
DATE 10/05/09 PLATE NO. R9-3BL.1

PROJECT NO:	HWY:	COUNTY:	SHEET NO: E
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7

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Use Size 2 for Sidewalks. Use Size 3 for Paths and Trails.



R9-9

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	12	1 3/4	1/2	1/2	3	2 1/8	1 3/4	10	8 1/8																	2.0
2M	24	12	1 3/4	1/2	1/2	3	2 1/8	1 3/4	10	8 1/8																	2.0
3	30	18	1 3/4	1/2	1/2	4	3 1/2	3	12 1/2	10 1/4																	3.75
4																											
5																											

STANDARD SIGN
R9-9

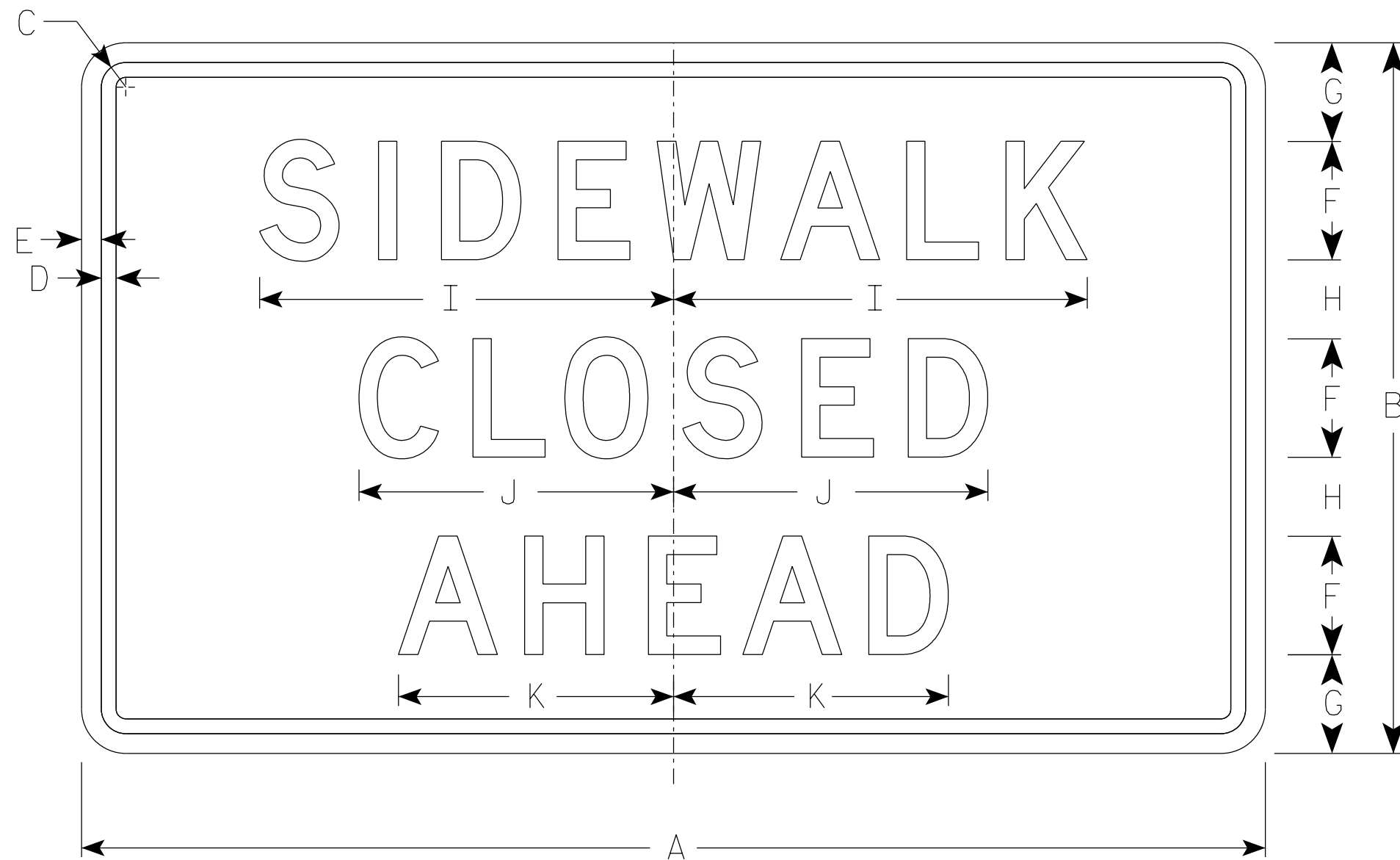
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 8/11/16 PLATE NO. R9-9.6

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



R9-9A

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30	18	1 1/8	3/8	1/2	3	2 1/2	2	10 1/2	8	7																3.75
2M	30	18	1 1/8	3/8	1/2	3	2 1/2	2	10 1/2	8	7																3.75
3																											
4																											
5																											

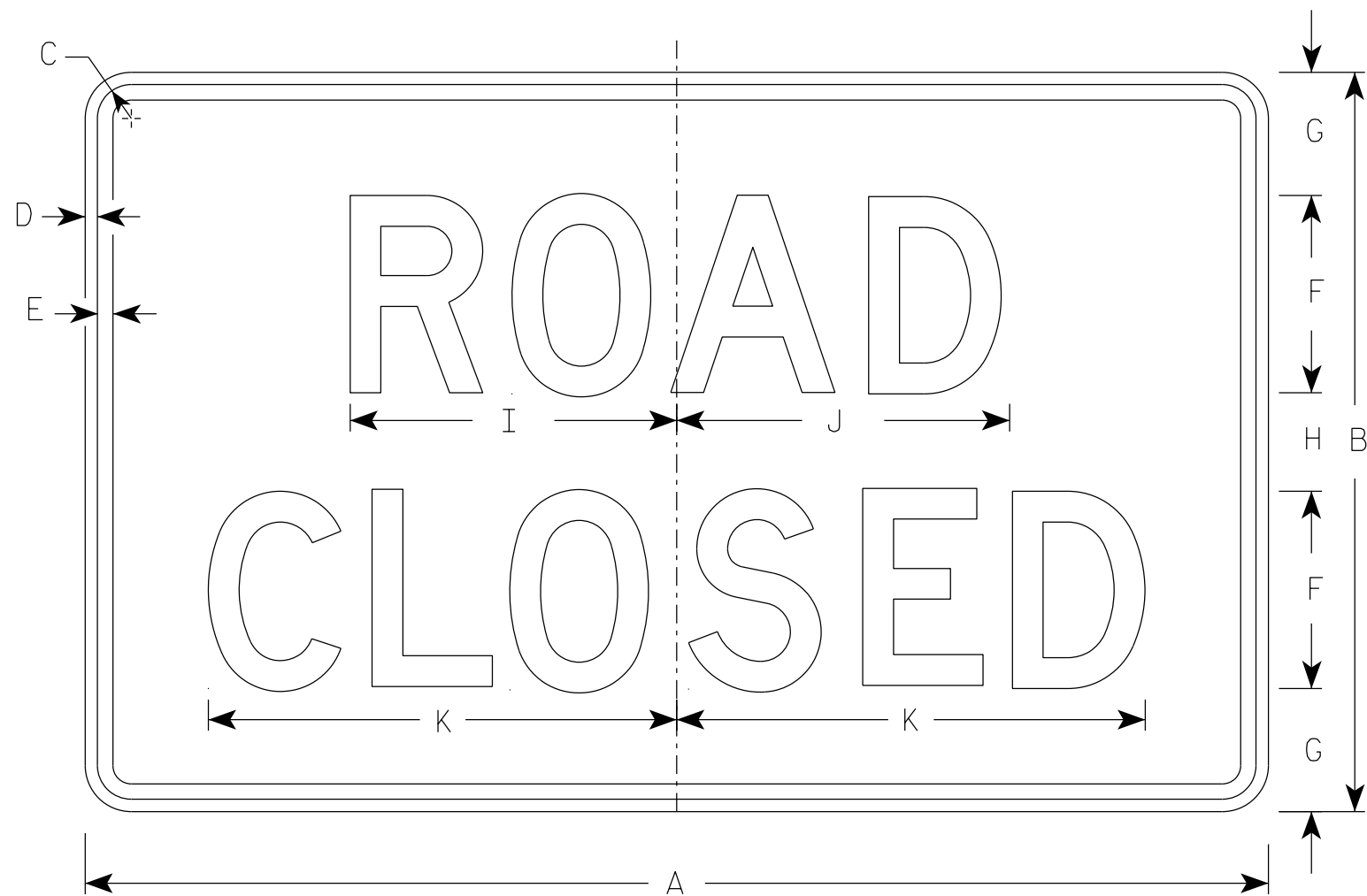
STANDARD SIGN
R9-9A

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 8/31/2020 PLATE NO. R9-9A.1

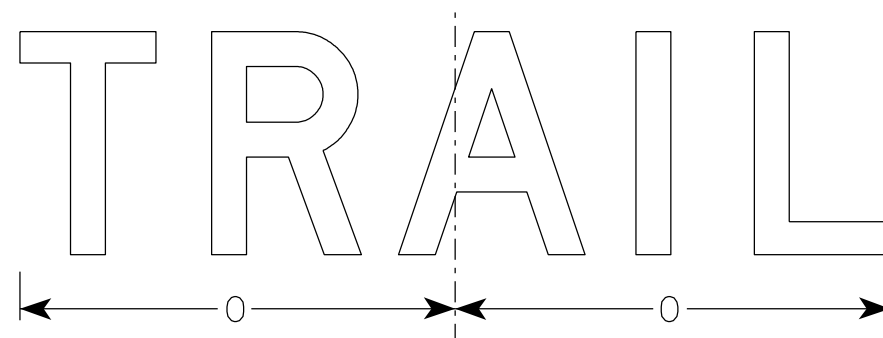
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**



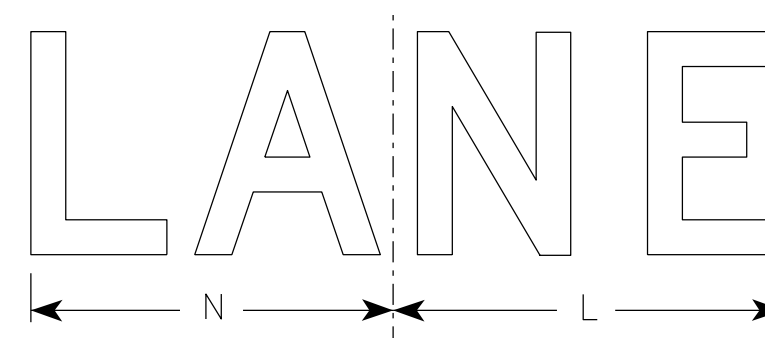
R11-2



R11-2R



R11-2T



R11-2L

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Modify the message as required.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
2M	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
3	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
4	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
5	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0

STANDARD SIGN
R11-2

WISCONSIN DEPT OF TRANSPORTATION

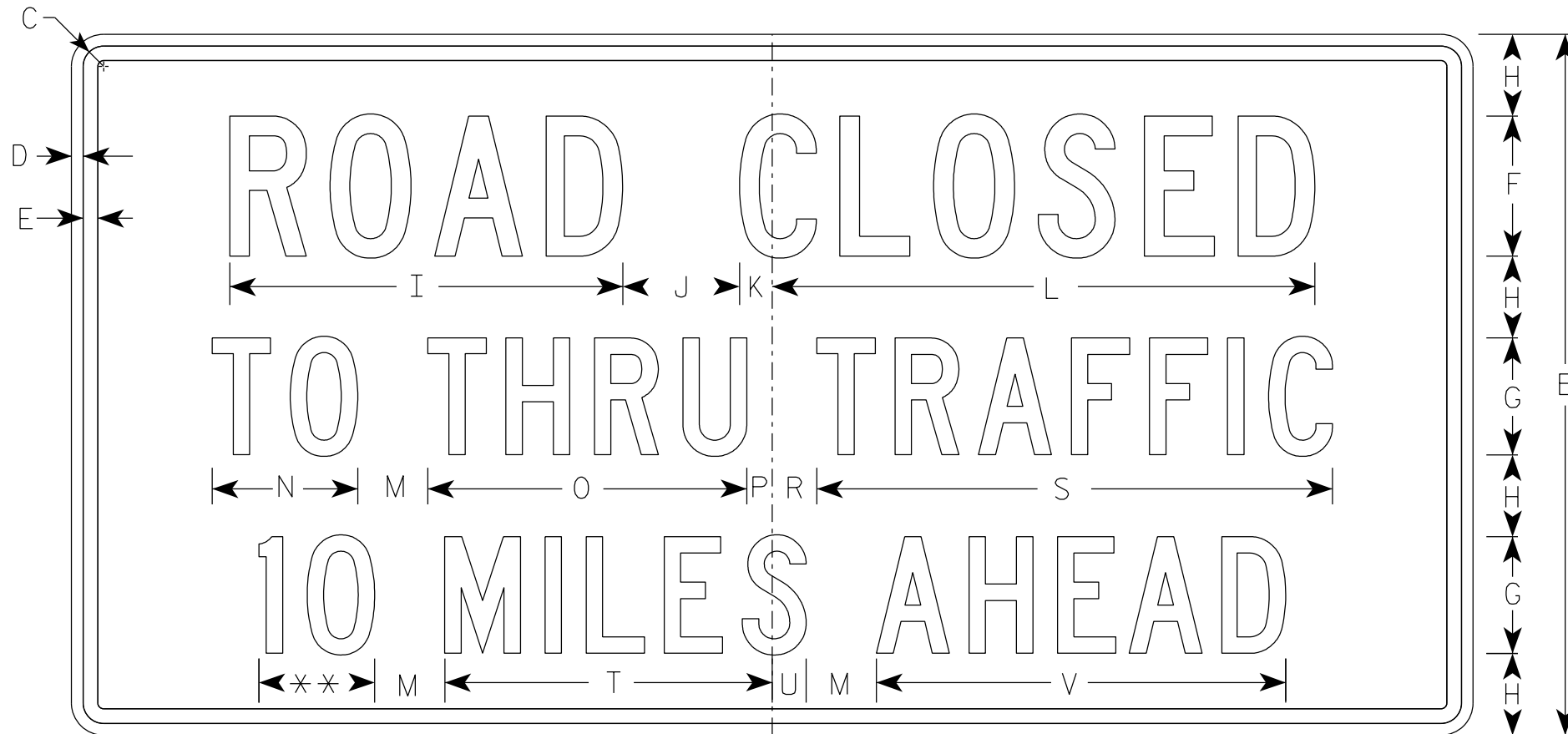
APPROVED *Matthew R Rauch*
For State Traffic Engineer

DATE 3/29/2021 PLATE NO. R11-2.11

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

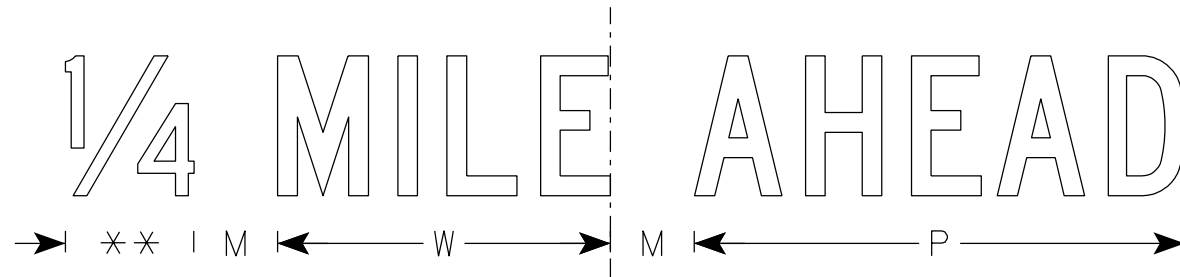
NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals to nearest quarter mile and optically adjust spacing to achieve proper balance.



R11-3

** See Note 5



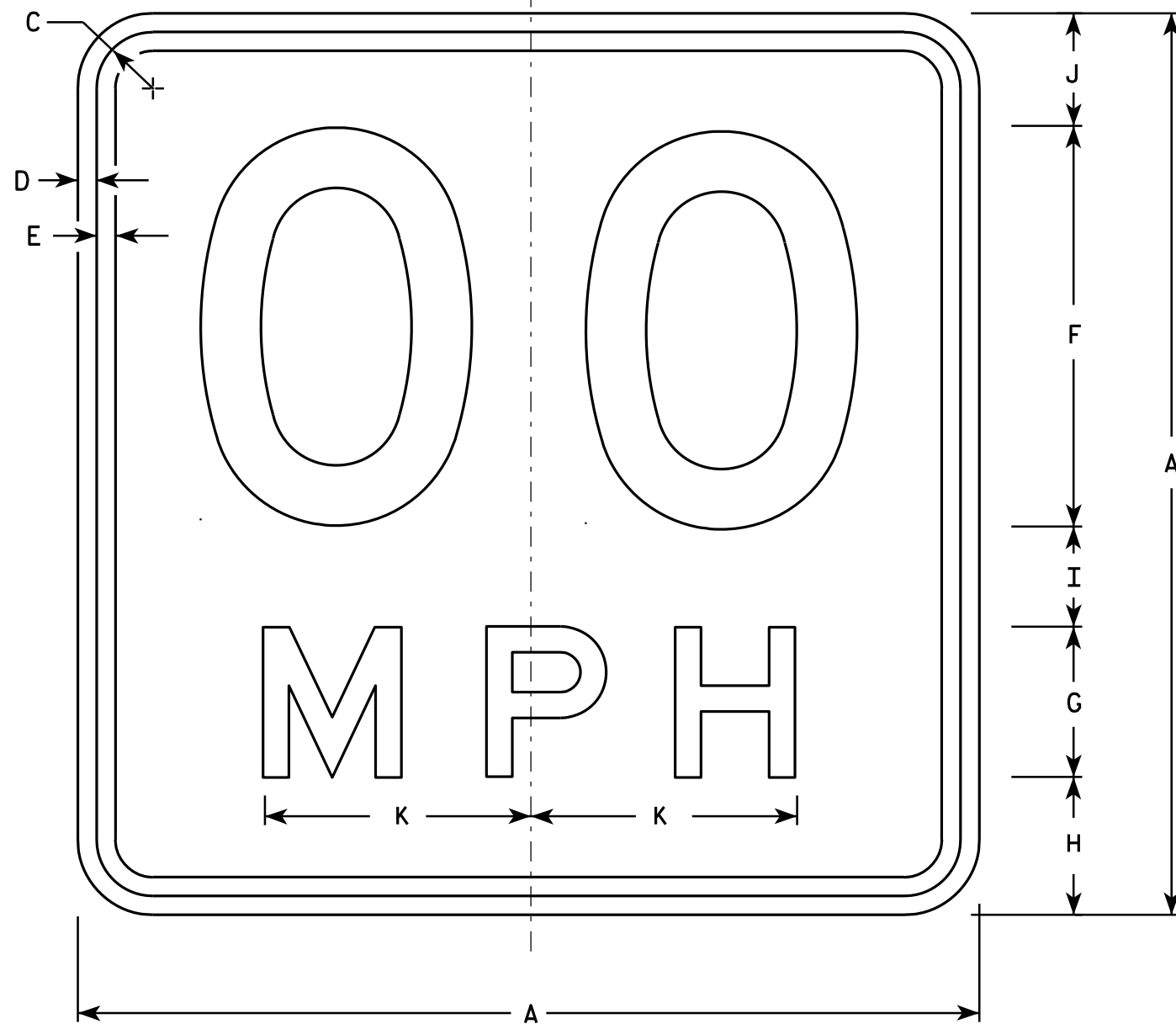
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	v	w	X	Y	Z	Area sq. ft.
1	36	18	1 1/4	3/8	3/8	4	3	2	11 1/4	3	1 1/8	15 3/8	2	3 3/4	8 1/4	5/8		1 3/8	13 1/4	8 3/8	7/8	10 1/2	7 1/8			4.5	
2S	60	30	1 3/8	1/2	5/8	6	5	3 1/2	16 7/8	5	1 3/8	23 1/4	3	6 1/4	13 5/8	1 1/8		1 7/8	22 1/8	14	1 1/2	17 1/2	11 7/8			12.5	
2M	60	30	1 3/8	1/2	5/8	6	5	3 1/2	16 7/8	5	1 3/8	23 1/4	3	6 1/4	13 5/8	1 1/8		1 7/8	22 1/8	14	1 1/2	17 1/2	11 7/8			12.5	
3																											
4																											
5																											

STANDARD SIGN
R11-3

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 6/14/2021 PLATE NO. R11-3.9



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Message Series - See Note 6
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals and optically space about centerline to achieve proper balance.
6. Line 1 is Series D
Line 2 is Series E

W13-1

* For 30" x 30" Warning Signs, use 18" x 18" W13-1 signs.
For 36" x 36" Warning Signs, use 24" x 24" W13-1 signs.

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18		1 1/8	3/8	3/8	8	3	2 3/4	2	2 1/4	5 3/8																2.25
* 2S	18		1 1/8	3/8	3/8	8	3	2 3/4	2	2 1/4	5 3/8																2.25
* 2M	18		1 1/8	3/8	3/8	8	3	2 3/4	2	2 1/4	5 3/8																2.25
3	24		1 1/8	3/8	1/2	10	4	4	2 3/4	3 1/4	6 5/8																4.00
4	36		1 5/8	5/8	3/4	16	6	5 1/2	4	4 1/2	10 5/8																9.00
5	36		1 5/8	5/8	3/4	16	6	5 1/2	4	4 1/2	10 5/8																9.00

STANDARD SIGN
W13-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 5/31/12 PLATE NO. W13-1.16

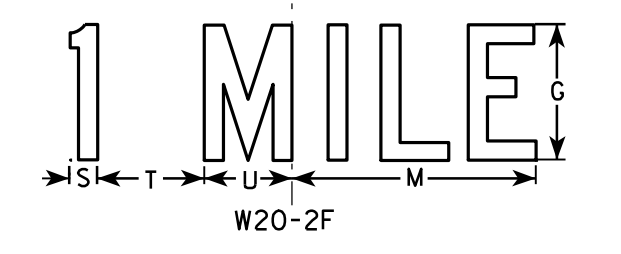
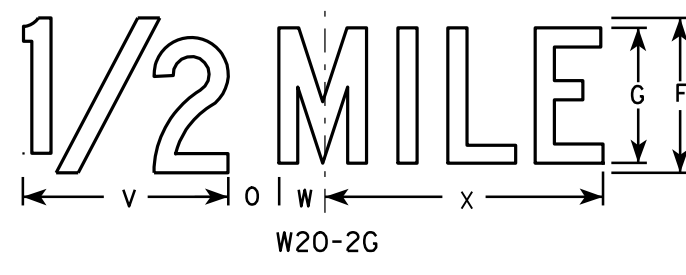
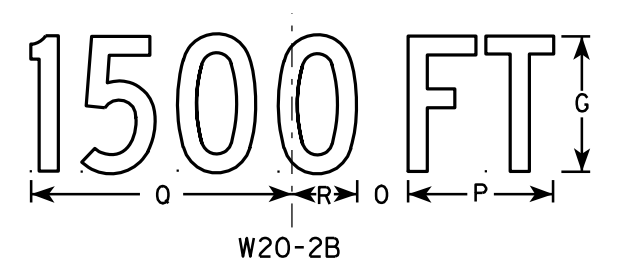
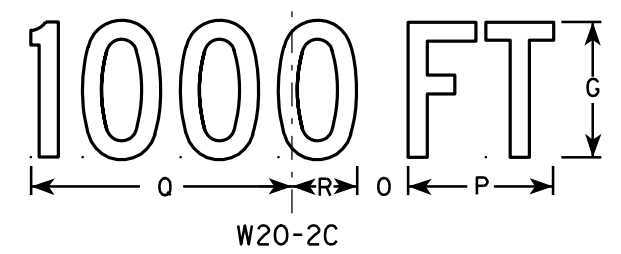
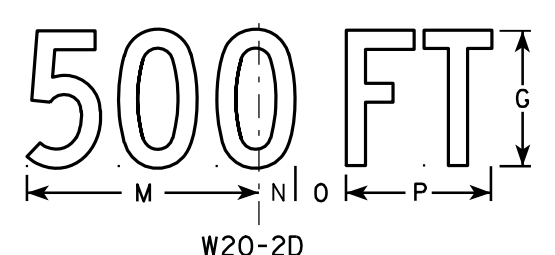
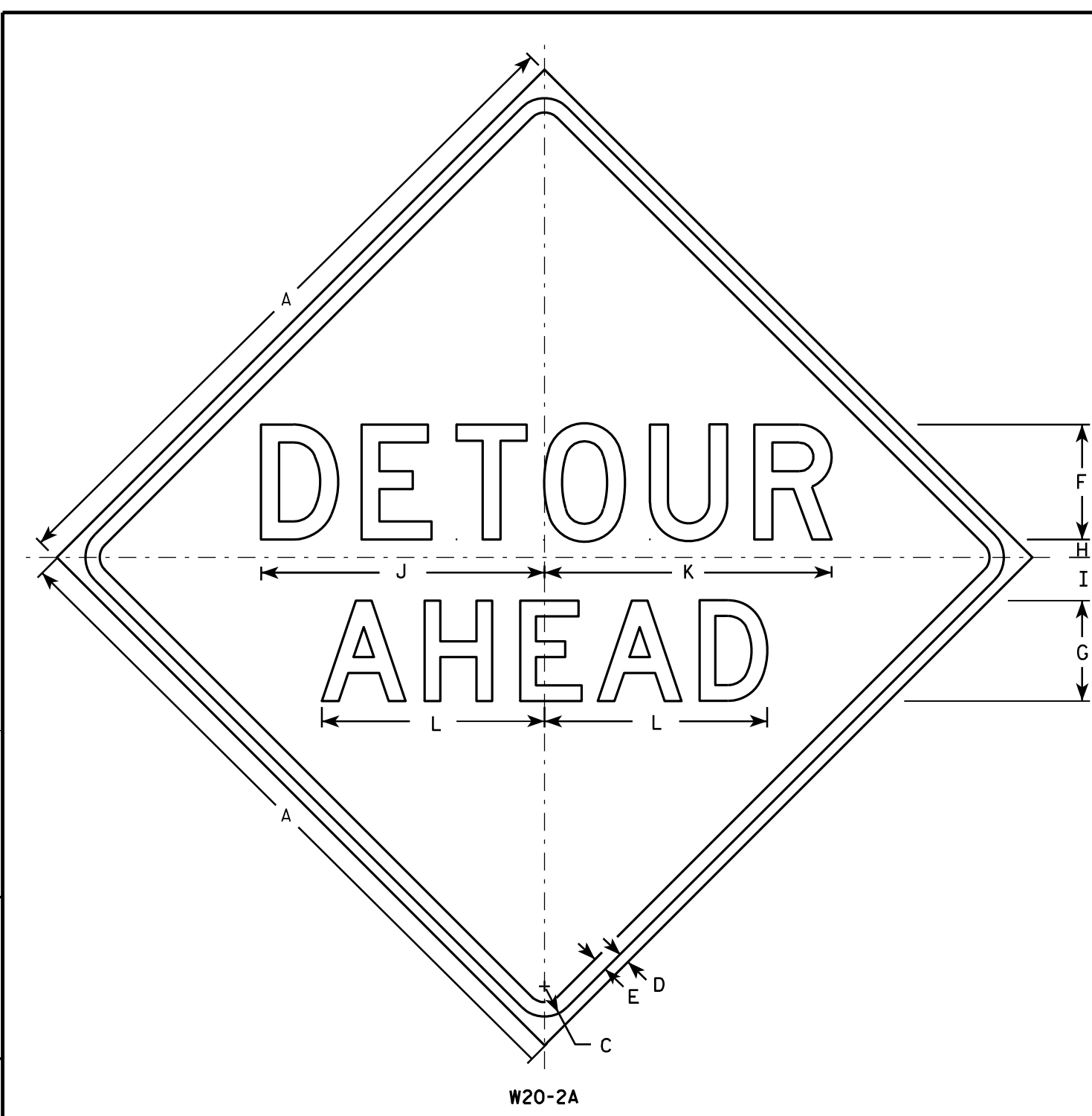
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - See note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Line 1 is Series D.
Line 2 is Series D for AHEAD and Series C for all other distances.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	6	5	1	2 1/4	14 3/4	15	11 5/8	9	1 3/8	1 7/8	5 5/8	10 1/8	2 1/2	1 1/8	4 1/2	3 1/2	8	1 3/4	10 3/4			9.0
2S	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
2M	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
3	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
4	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
5	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0

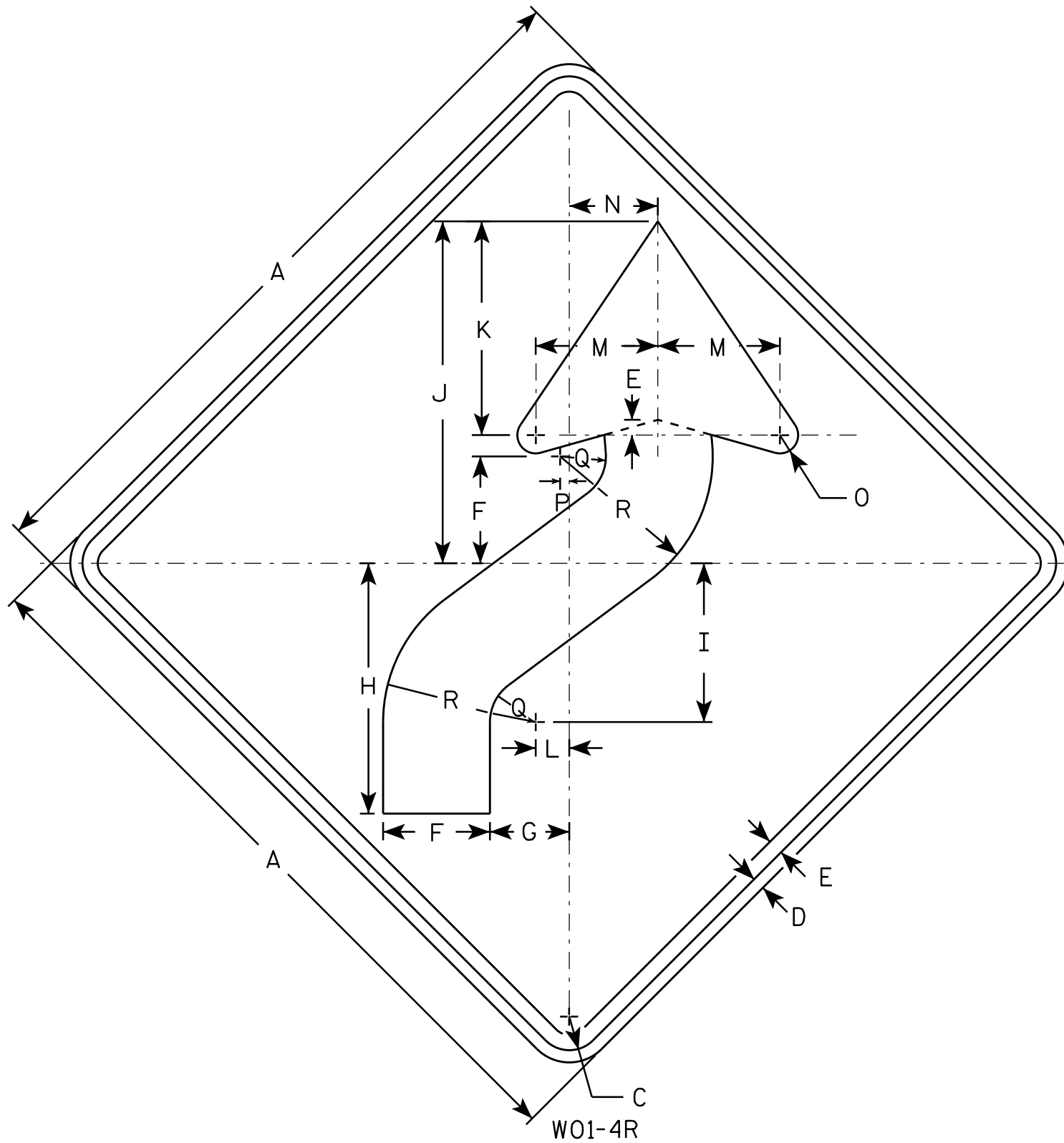
STANDARD SIGN
W20-2A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Raub*
for State Traffic Engineer

DATE 3/18/11 PLATE NO. W20-2.6

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. W01-4L is the same as W01-4R except the arrow is reversed along the vertical centerline.

7

7

W01-4R

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	5 1/4	4	12 3/8	7 7/8	16 7/8	10 1/2	1 5/8	6	4 1/2	1	1/2	2 1/4	7 1/2									9.0
2S	48		2 1/4	3/4	1	7	5 1/4	16 1/2	10 1/2	22 1/2	14	2 1/4	8	6	1 1/4	5/8	3	10									16.0
2M	48		2 1/4	3/4	1	7	5 1/4	16 1/2	10 1/2	22 1/2	14	2 1/4	8	6	1 1/4	5/8	3	10									16.0
3	48		2 1/4	3/4	1	7	5 1/4	16 1/2	10 1/2	22 1/2	14	2 1/4	8	6	1 1/4	5/8	3	10									16.0
4	48		2 1/4	3/4	1	7	5 1/4	16 1/2	10 1/2	22 1/2	14	2 1/4	8	6	1 1/4	5/8	3	10									16.0
5	48		2 1/4	3/4	1	7	5 1/4	16 1/2	10 1/2	22 1/2	14	2 1/4	8	6	1 1/4	5/8	3	10									16.0

STANDARD SIGN
W01-4

WISCONSIN DEPT OF TRANSPORTATION

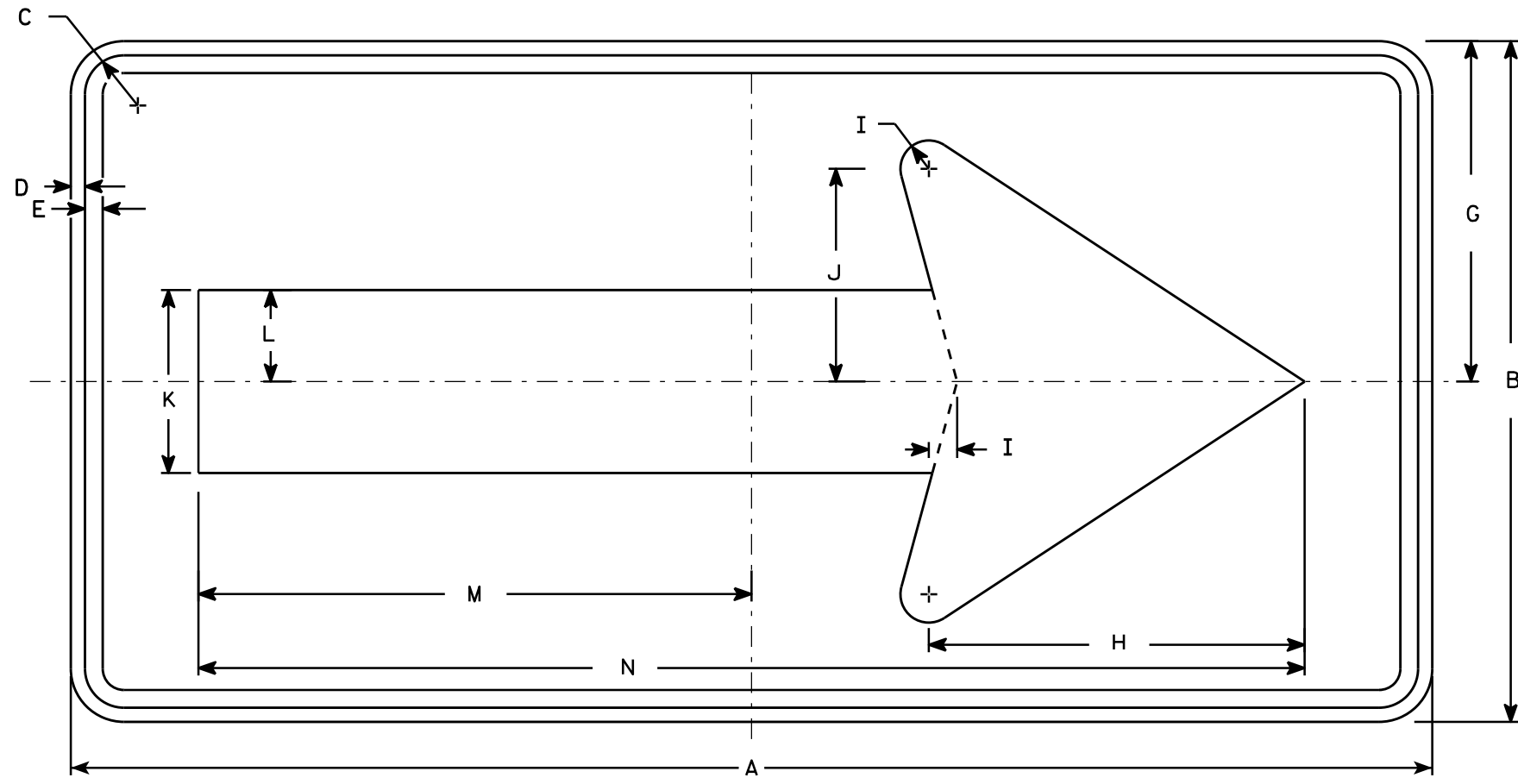
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 11/18/13 PLATE NO. W01-4.1

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



W01-6

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SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48	24	1 3/8	1/2	5/8		12	13 1/4	1	7 1/2	6 1/2	3 1/4	19 1/2	39													8.0
2M	48	24	1 3/8	1/2	5/8		12	13 1/4	1	7 1/2	6 1/2	3 1/4	19 1/2	39													8.0
3	60	30	1 3/8	1/2	5/8		15	16 1/4	1 1/4	9 1/4	8	4	24 3/8	48 3/4													12.5
4	60	30	1 3/8	1/2	5/8		15	16 1/4	1 1/4	9 1/4	8	4	24 3/8	48 3/4													12.5
5	60	30	1 3/8	1/2	5/8		15	16 1/4	1 1/4	9 1/4	8	4	24 3/8	48 3/4													12.5

STANDARD SIGN
W01-6

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matthew R. Rauch
for State Traffic Engineer

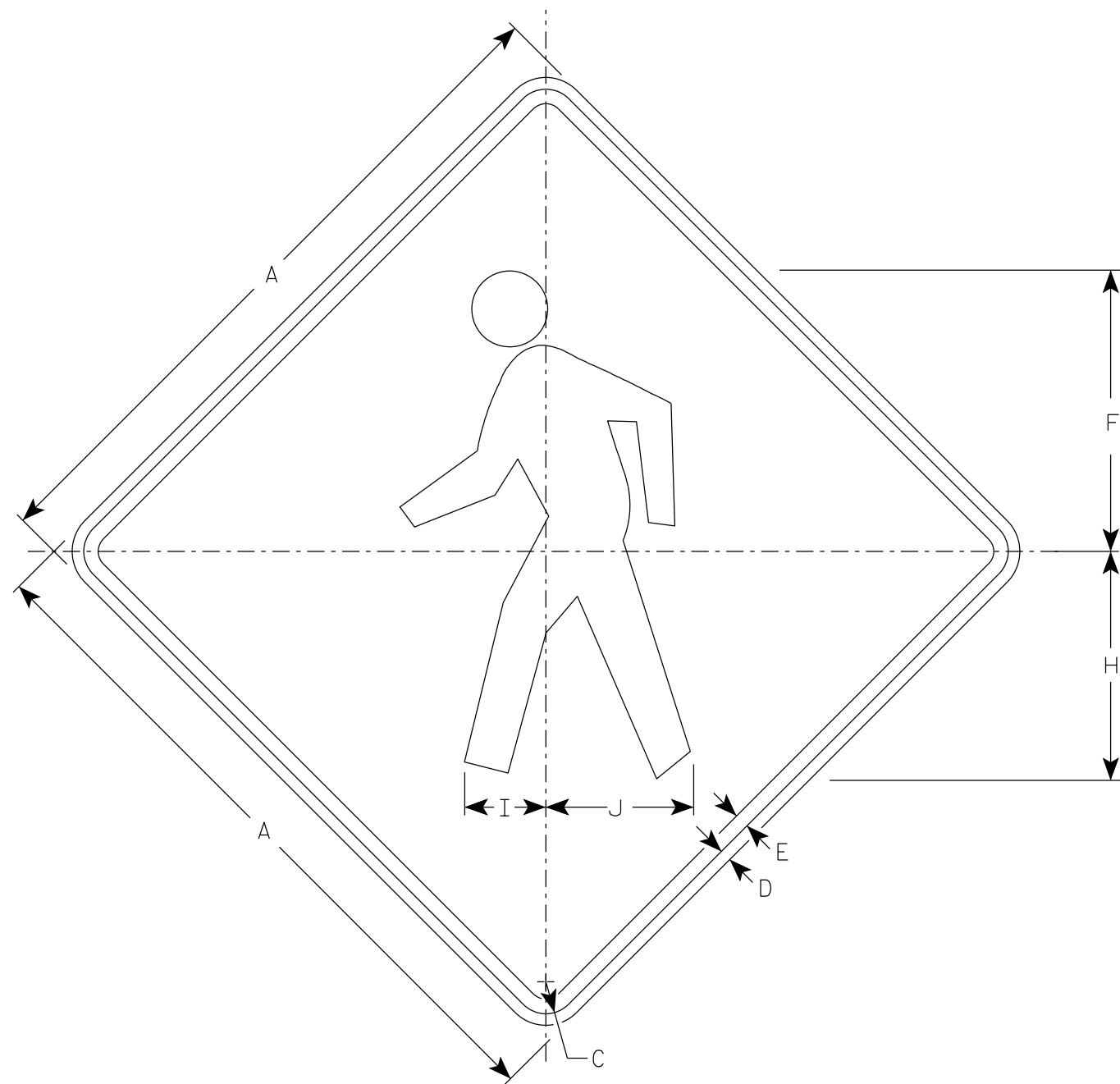
DATE 11/18/13

PLATE NO. W01-6.1

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



W011-2

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	14 1/2		11 7/8	4 1/4	7 5/8																	9.0
2S	48		2 1/4	3/4	1	19 3/8		15 3/4	5 5/8	10 1/4																	16.0
2M	48		2 1/4	3/4	1	19 3/8		15 3/4	5 5/8	10 1/4																	16.0
3	48		2 1/4	3/4	1	19 3/8		15 3/4	5 5/8	10 1/4																	16.0
4	48		2 1/4	3/4	1	19 3/8		15 3/4	5 5/8	10 1/4																	16.0
5																											

STANDARD SIGN
W011-2

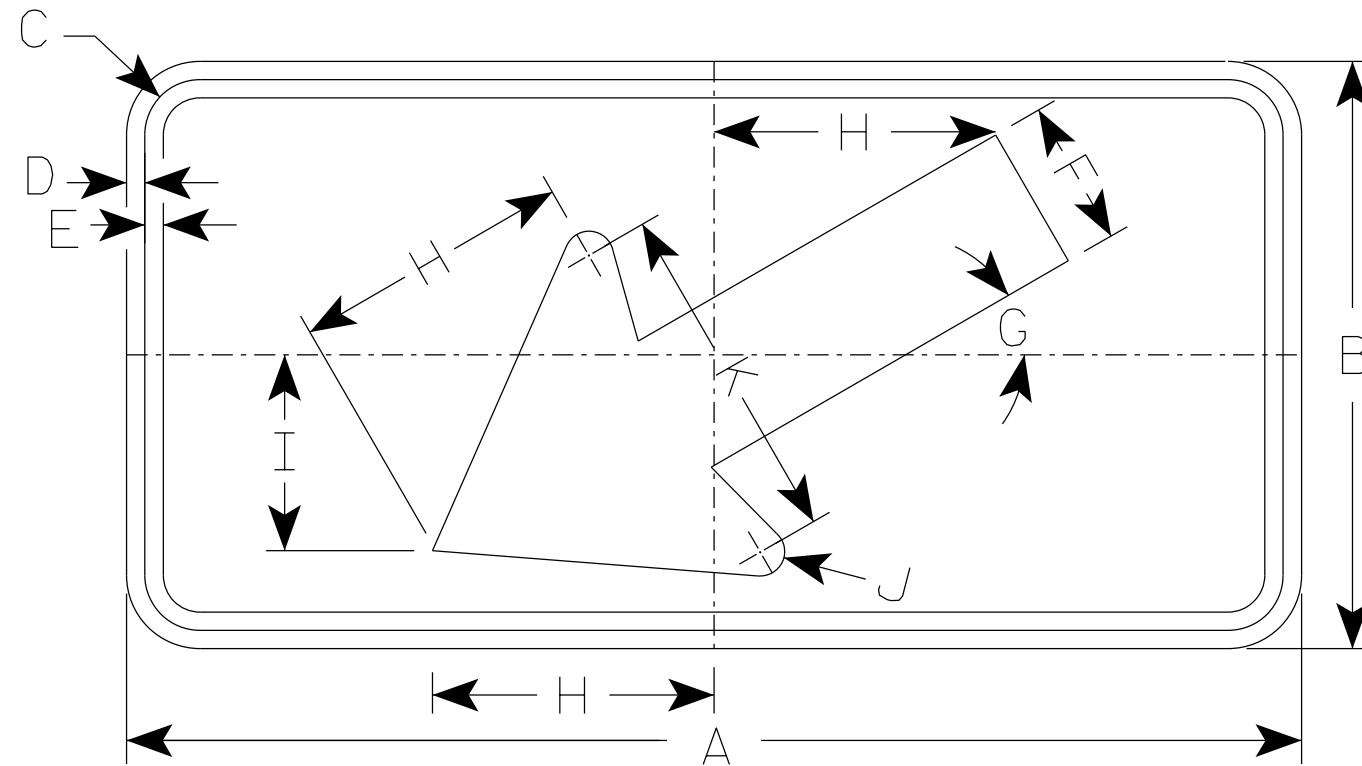
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 4/8/2020 PLATE NO. W011-2.1

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Corners may be square or rounded but corners shall be rounded when base material is metal.
4. W016-7R is the same as W016-L except the arrow is reversed along the vertical centerline.



W016-7L

7

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SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30	18	1 1/8	3/8	1/2	4 1/2	30°	8 1/2	6	5/8	10 1/4																3.75
2S	48	24	1 3/8	1/2	5/8	6	30°	11 1/2	8	1	14																8.0
2M	48	24	1 3/8	1/2	5/8	6	30°	11 1/2	8	1	14																8.0
3	48	24	1 3/8	1/2	5/8	6	30°	11 1/2	8	1	14																8.0
4	48	24	1 3/8	1/2	5/8	6	30°	11 1/2	8	1	14																8.0
5	48	24	1 3/8	1/2	5/8	6	30°	11 1/2	8	1	14																8.0

STANDARD SIGN
W016-7

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Rauch*
for State Traffic Engineer

DATE 3/16/2021 PLATE NO. W016-7.2

PROJECT NO:	HWY:	COUNTY:	SHEET NO:	E
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STATION	DISTANCE	AREA (SF)	INCREMENTAL VOL (CY) (UNADJUSTED)	CUMULATIVE VOL (CY)	
		CUT	CUT NOTE 1	CUT NOTE 1	MASS ORDINATE
81+60	0.00	86.90	0.00	0.00	0.00
81+99.197	39.20	78.41	120.00	120.00	120.00
81+99.22	0.02	83.43	0.00	120.00	120.00
82+00	0.78	82.60	2.00	122.00	122.00
82+50	50.00	78.41	149.00	271.00	271.00
83+00	50.00	78.32	145.00	416.00	416.00
83+50	50.00	79.96	147.00	563.00	563.00
84+00	50.00	81.14	149.00	712.00	712.00
84+50	50.00	81.25	150.00	862.00	862.00
85+00	50.00	80.74	150.00	1012.00	1012.00
85+10.701	10.70	84.50	33.00	1045.00	1045.00
85+10.725	0.02	79.48	0.00	1045.00	1045.00
85+39.255	28.53	78.63	84.00	1129.00	1129.00
85+50	10.75	78.68	31.00	1160.00	1160.00
85+64.222	14.22	79.10	42.00	1202.00	1202.00
85+64.246	0.02	84.14	0.00	1202.00	1202.00
85+79.094	14.85	83.64	46.00	1248.00	1248.00
85+79.117	0.02	78.61	0.00	1248.00	1248.00
86+00	20.88	78.96	61.00	1309.00	1309.00
86+50	50.00	79.09	146.00	1455.00	1455.00
86+59.256	9.26	79.15	27.00	1482.00	1482.00
86+59.279	0.02	84.21	0.00	1482.00	1482.00
87+00	40.72	79.55	123.00	1605.00	1605.00
87+50	50.00	80.27	148.00	1753.00	1753.00
87+60	10.00	78.85	29.00	1782.00	1782.00
106+30	0.00	115.23	0.00	1782.00	1782.00
106+50	20.00	116.98	86.00	1868.00	1868.00
106+96.647	46.65	117.53	203.00	2071.00	2071.00
107+00	3.35	108.53	14.00	2085.00	2085.00
107+06.398	6.40	97.30	24.00	2109.00	2109.00
107+18.45	12.05	90.89	42.00	2151.00	2151.00
107+33.875	15.43	90.49	52.00	2203.00	2203.00
107+33.889	0.01	95.58	0.00	2203.00	2203.00
107+50	16.11	96.64	57.00	2260.00	2260.00
108+00	50.00	96.28	179.00	2439.00	2439.00
108+50	50.00	85.74	169.00	2608.00	2608.00
109+00	50.00	77.84	151.00	2759.00	2759.00
109+50	50.00	79.17	145.00	2904.00	2904.00
110+00	50.00	83.85	151.00	3055.00	3055.00
110+50	50.00	79.59	151.00	3206.00	3206.00
110+63.437	13.44	77.37	39.00	3245.00	3245.00
111+00	36.56	79.44	106.00	3351.00	3351.00
111+50	50.00	79.27	147.00	3498.00	3498.00
112+00	50.00	76.88	145.00	3643.00	3643.00
112+50	50.00	80.59	146.00	3789.00	3789.00
113+00	50.00	82.09	151.00	3940.00	3940.00

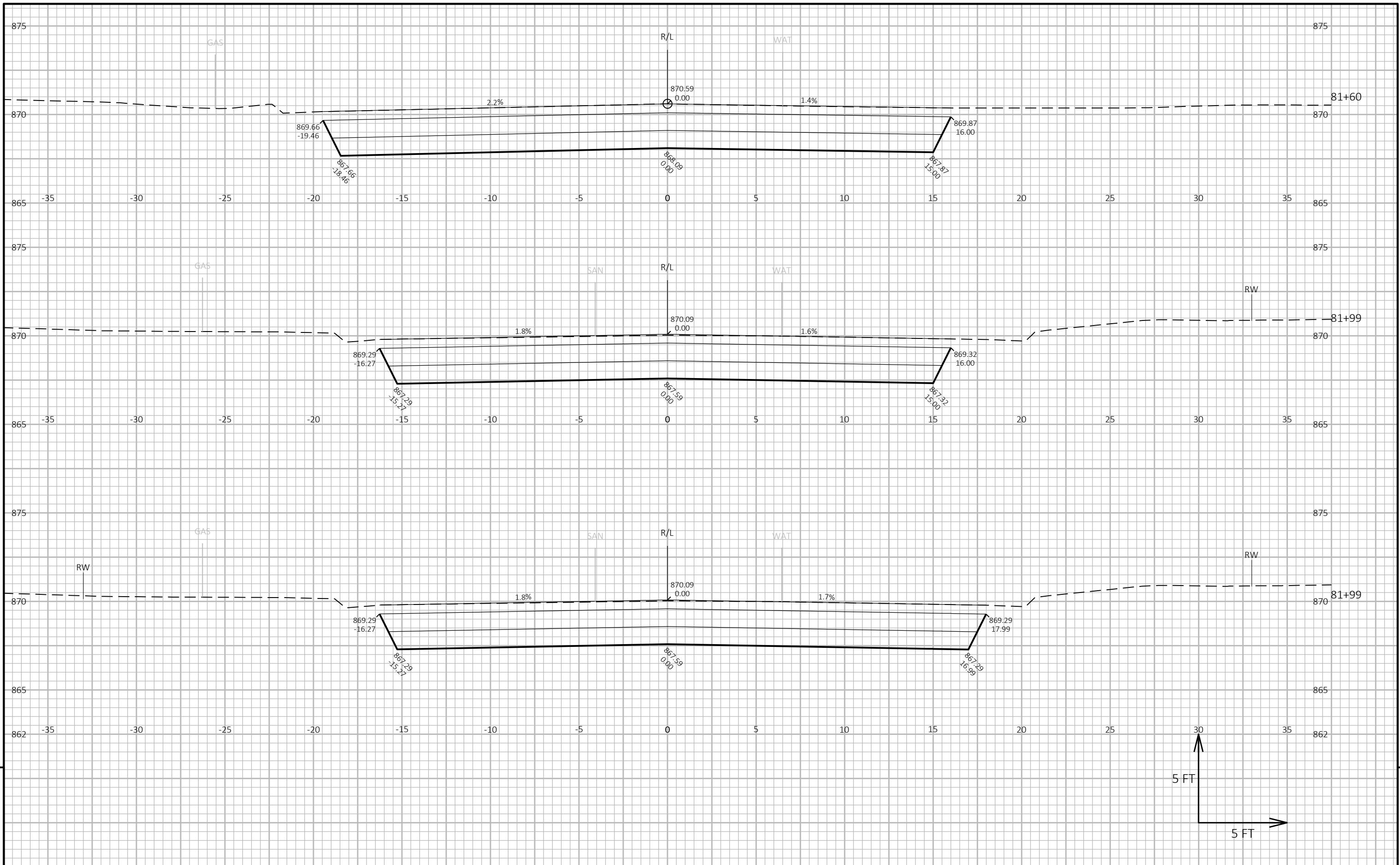
Notes:
1 - CUT CUT INCLUDES SALVAGED/UNUSABLE PAVEMENT MATERIAL

STATION	DISTANCE	AREA (SF)	INCREMENTAL VOL (CY) (UNADJUSTED)	CUMULATIVE VOL (CY)	
		CUT	CUT NOTE 1	CUT NOTE 1	MASS ORDINATE
119+50	0.00	79.88	0.00	3940.00	3940.00
120+00	50.00	84.00	152.00	4092.00	4092.00
120+00.537	0.54	84.49	2.00	4094.00	4094.00
120+00.561	0.02	79.45	0.00	4094.00	4094.00
120+50	49.44	80.15	146.00	4240.00	4240.00
121+00	50.00	81.42	150.00	4390.00	4390.00
121+50	50.00	80.18	150.00	4540.00	4540.00
122+00	50.00	79.06	147.00	4687.00	4687.00
122+50	50.00	79.90	147.00	4834.00	4834.00
122+54.731	4.73	83.96	14.00	4848.00	4848.00
122+54.756	0.02	78.93	0.00	4848.00	4848.00
122+88.22	33.46	79.14	98.00	4946.00	4946.00
122+88.263	0.04	84.25	0.00	4946.00	4946.00
123+00	11.74	77.97	35.00	4981.00	4981.00
123+50	50.00	78.51	145.00	5126.00	5126.00
124+00	50.00	78.93	146.00	5272.00	5272.00
124+12.76	12.76	84.85	39.00	5311.00	5311.00
124+12.784	0.02	79.83	0.00	5311.00	5311.00
124+50	37.22	78.92	109.00	5420.00	5420.00
124+51.125	1.13	78.78	3.00	5423.00	5423.00
124+71.788	20.66	77.59	60.00	5483.00	5483.00
125+00	28.21	78.32	81.00	5564.00	5564.00
125+01.92	1.92	78.28	6.00	5570.00	5570.00
125+01.943	0.02	83.29	0.00	5570.00	5570.00
125+50	48.06	77.41	143.00	5713.00	5713.00
125+83	33.00	78.13	95.00	5808.00	5808.00
10+14.928	0.00	16.85	0.00	5808.00	5808.00
10+20	5.07	18.05	3.00	5811.00	5811.00
10+25	5.00	21.08	4.00	5815.00	5815.00
10+30	5.00	26.14	4.00	5819.00	5819.00
10+40	10.00	36.53	12.00	5831.00	5831.00
10+48.901	8.90	37.75	12.00	5843.00	5843.00
10+50	1.10	37.68	2.00	5845.00	5845.00
10+52.799	2.80	37.76	4.00	5849.00	5849.00
10+59.047	6.25	36.17	9.00	5858.00	5858.00
10+60	0.95	35.02	1.00	5859.00	5859.00
10+69.322	9.32	26.92	11.00	5870.00	5870.00
10+71.363	2.04	23.72	2.00	5872.00	5872.00
10+80	8.64	19.18	7.00	5879.00	5879.00
10+83.167	3.17	19.06	2.00	5881.00	5881.00

Notes:	
1 - CUT	CUT INCLUDES SALVAGED/UNUSABLE PAVEMENT MATERIAL

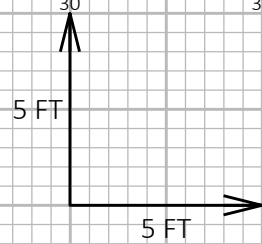
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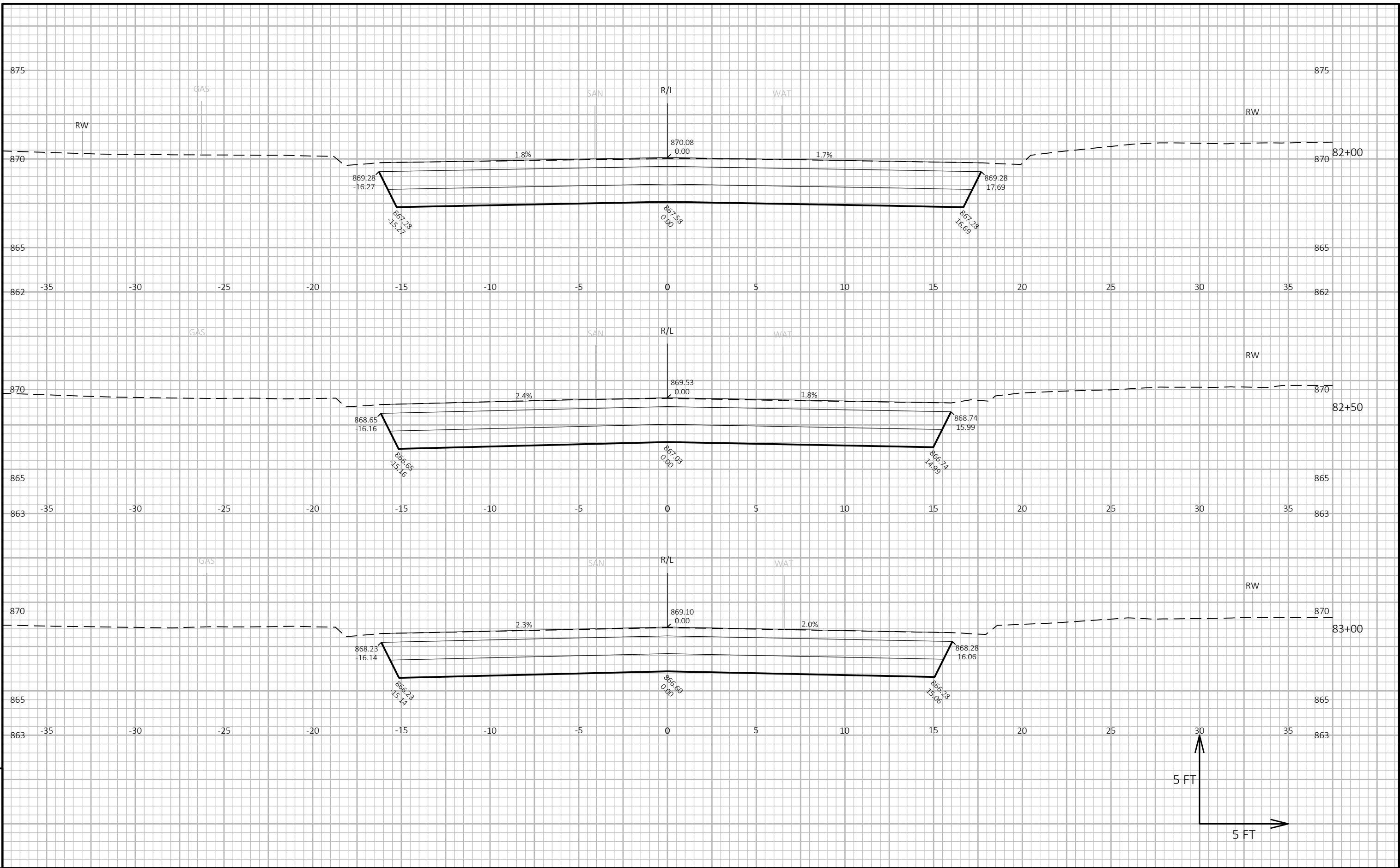


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PROJECT NO: 3070-04-60	HWY: STH 73	COUNTY: DANE	CROSS SECTIONS: STH 73	SHEET	E
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PROJECT NO: 3070-04-60

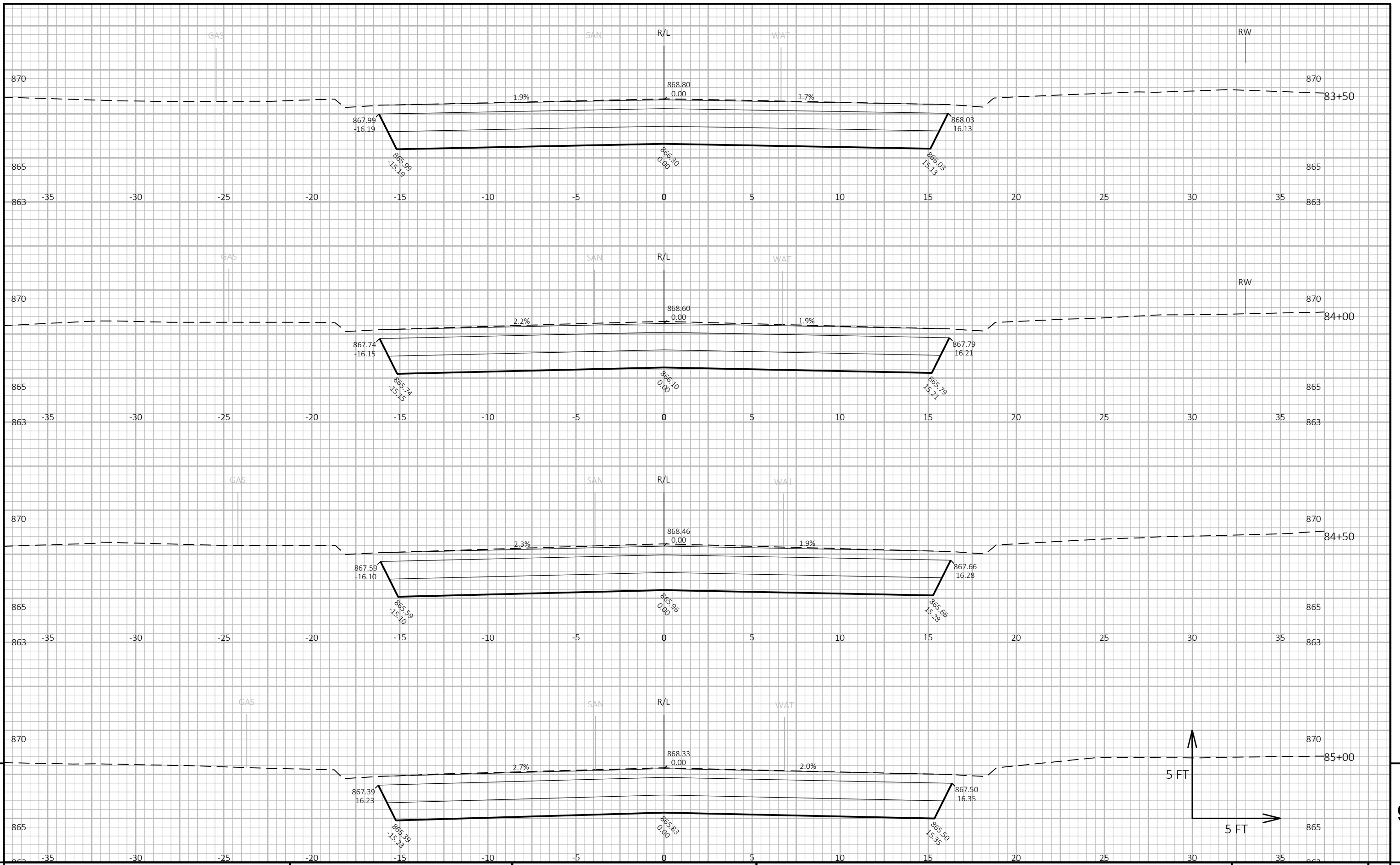
HWY: STH 73

COUNTY: DANE

CROSS SECTIONS: STH 73

SHEET

E

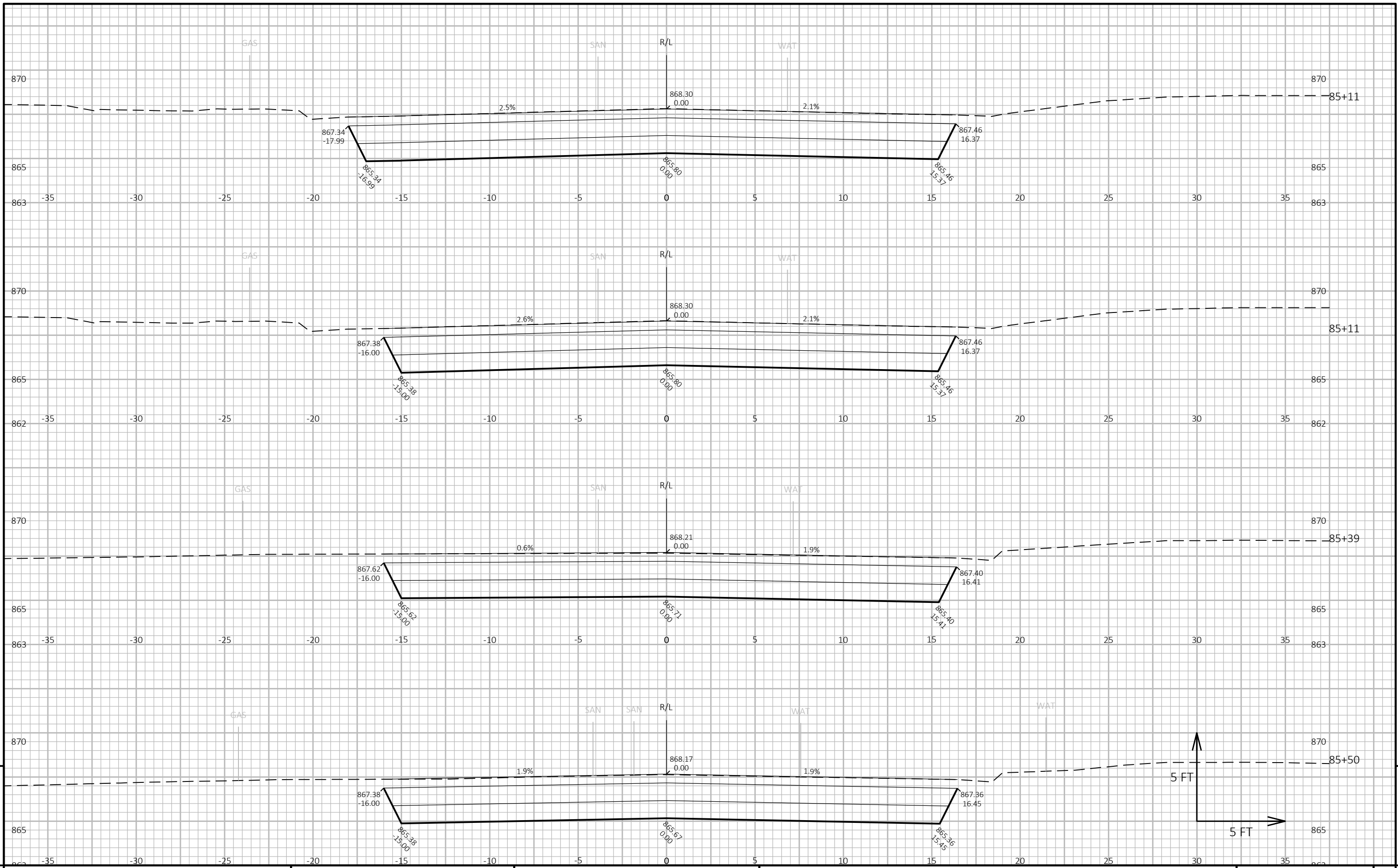


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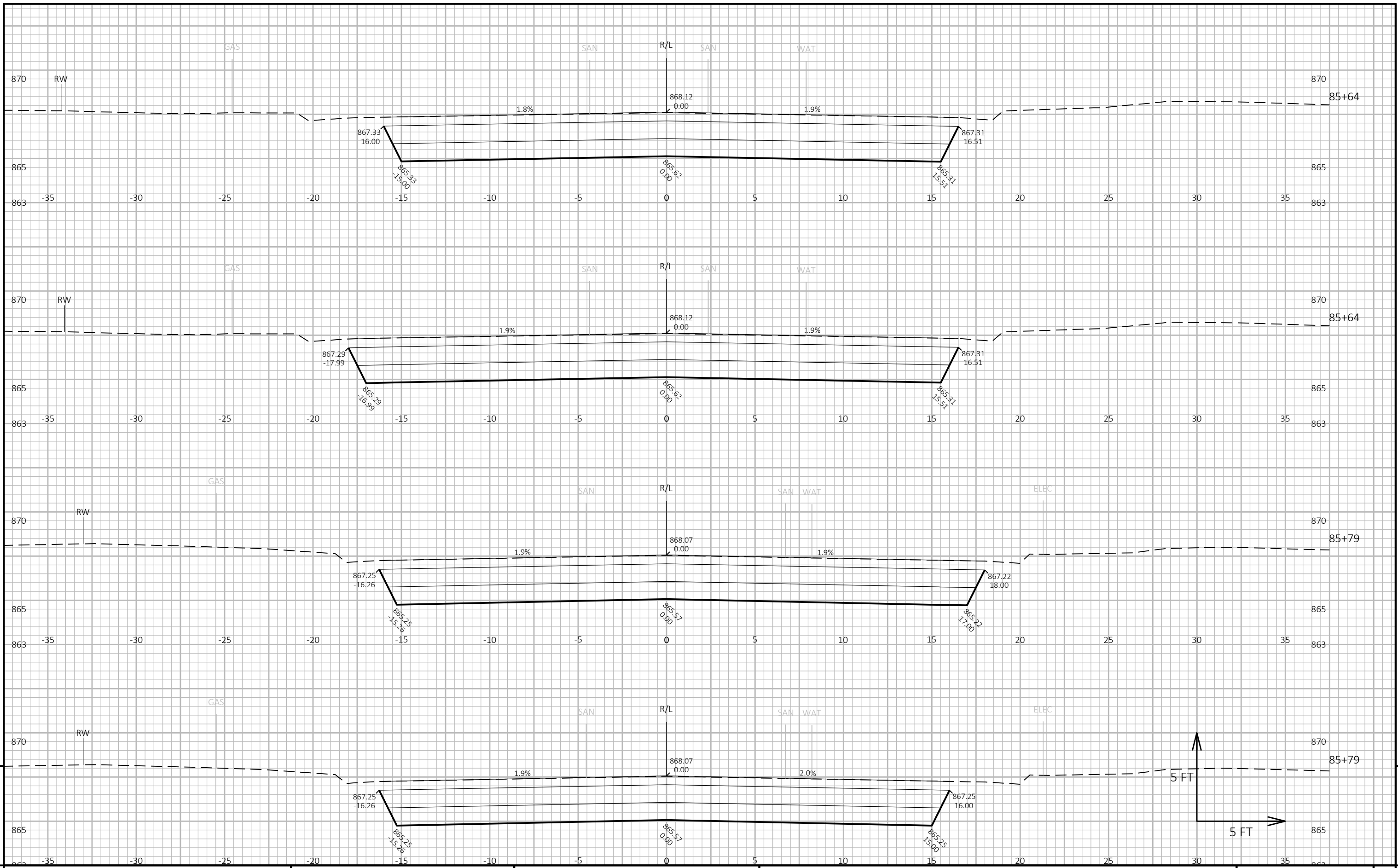
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PROJECT NO: 3070-04-60 HWY: STH 73 COUNTY: DANE CROSS SECTIONS: STH 73 SHEET E

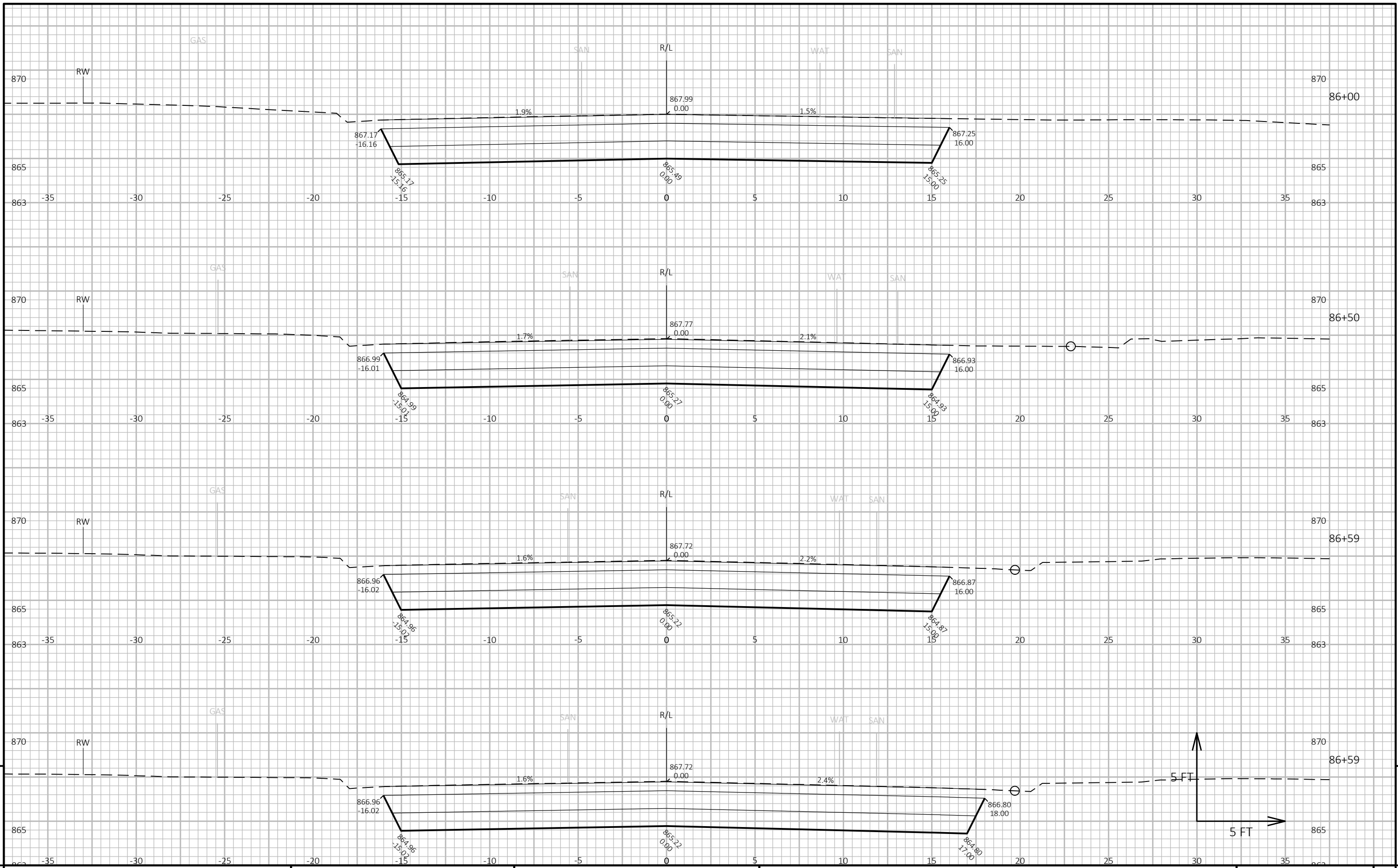
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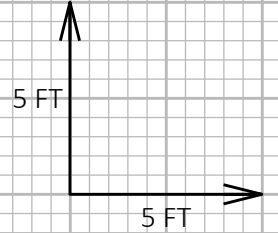
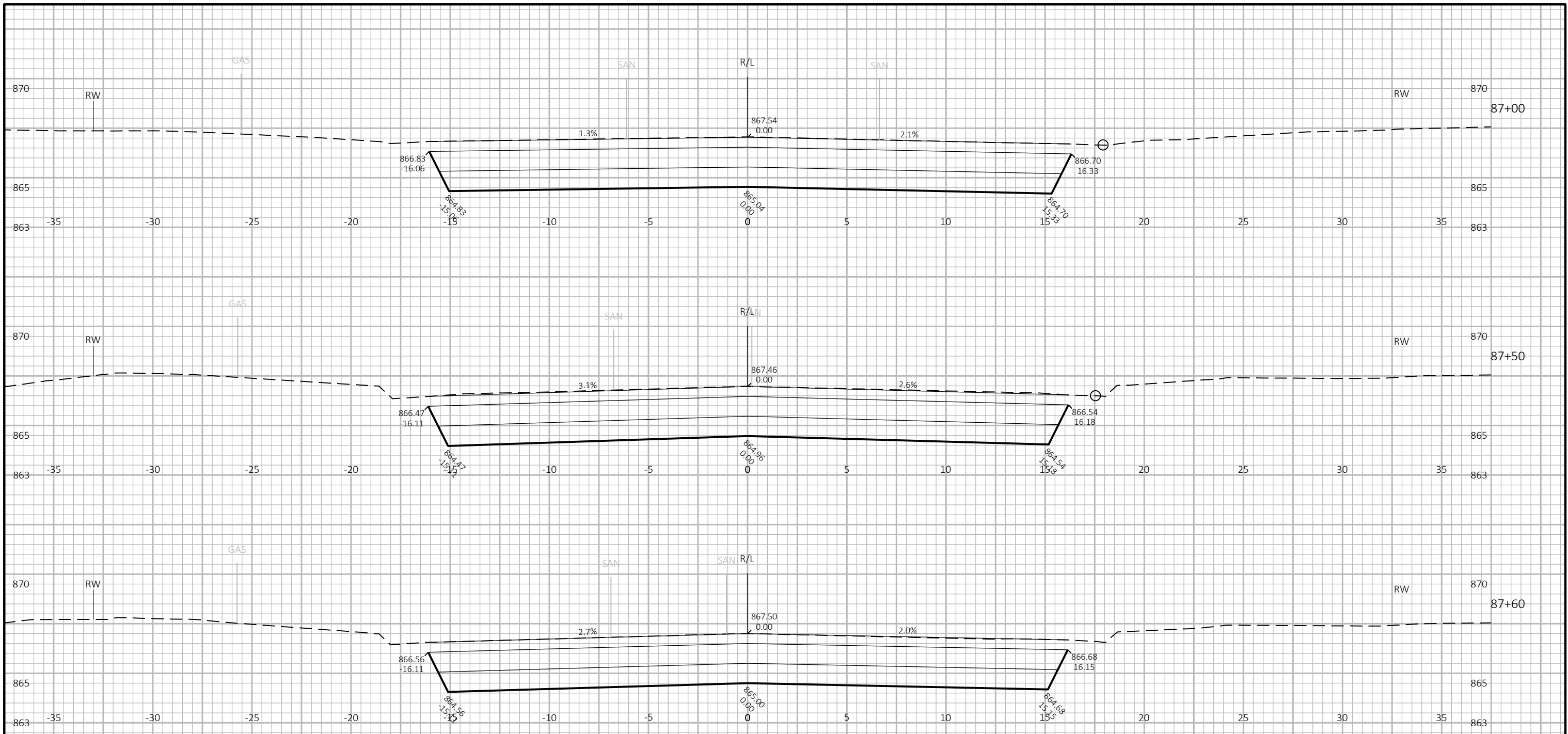
PROJECT NO: 3070-04-60 HWY: STH 73 COUNTY: DANE CROSS SECTIONS: STH 73 SHEET E



PROJECT NO: 3070-04-60 HWY: STH 73 COUNTY: DANE CROSS SECTIONS: STH 73 SHEET E



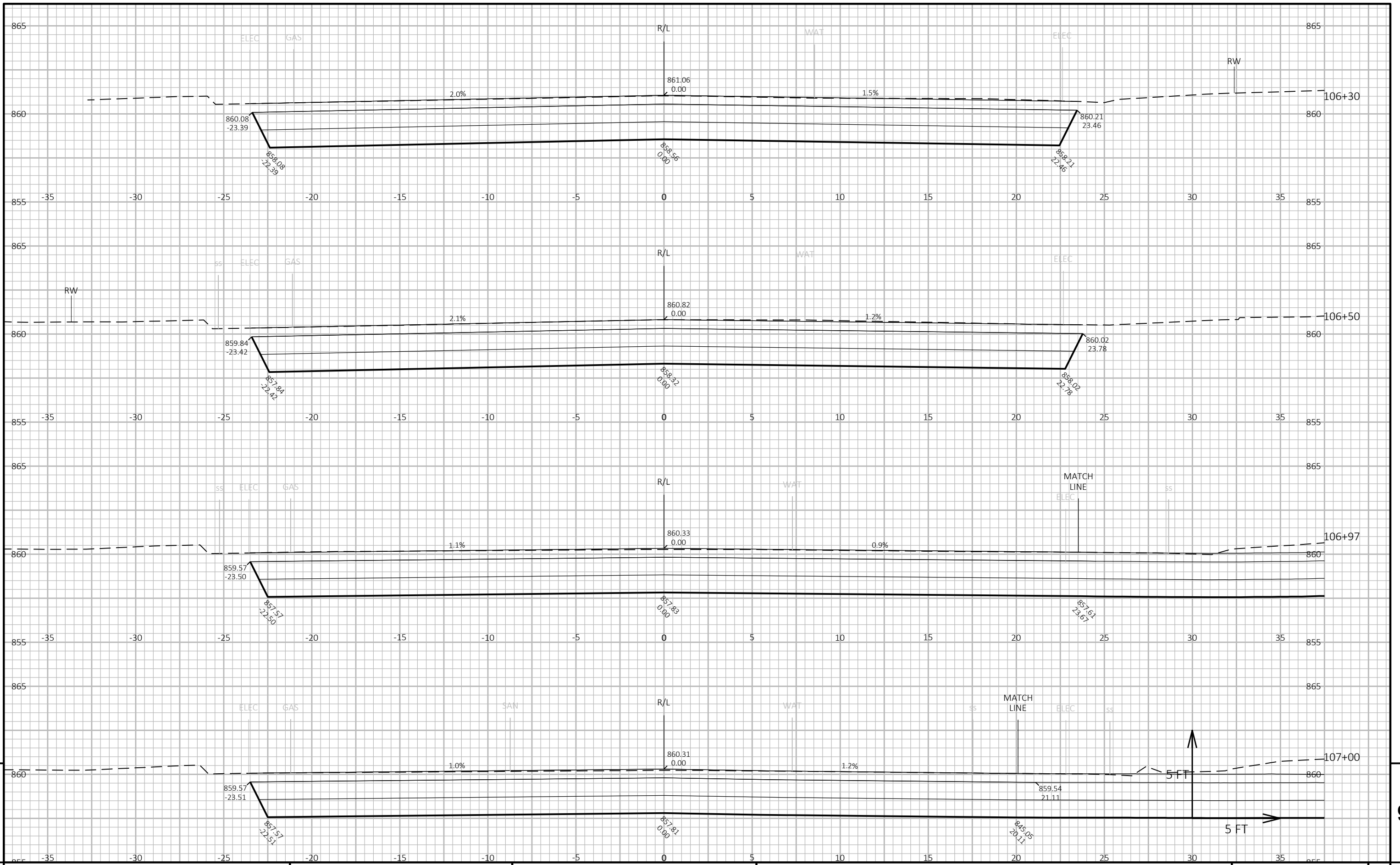
PROJECT NO: 3070-04-60	HWY: STH 73	COUNTY: DANE	CROSS SECTIONS: STH 73	SHEET	E
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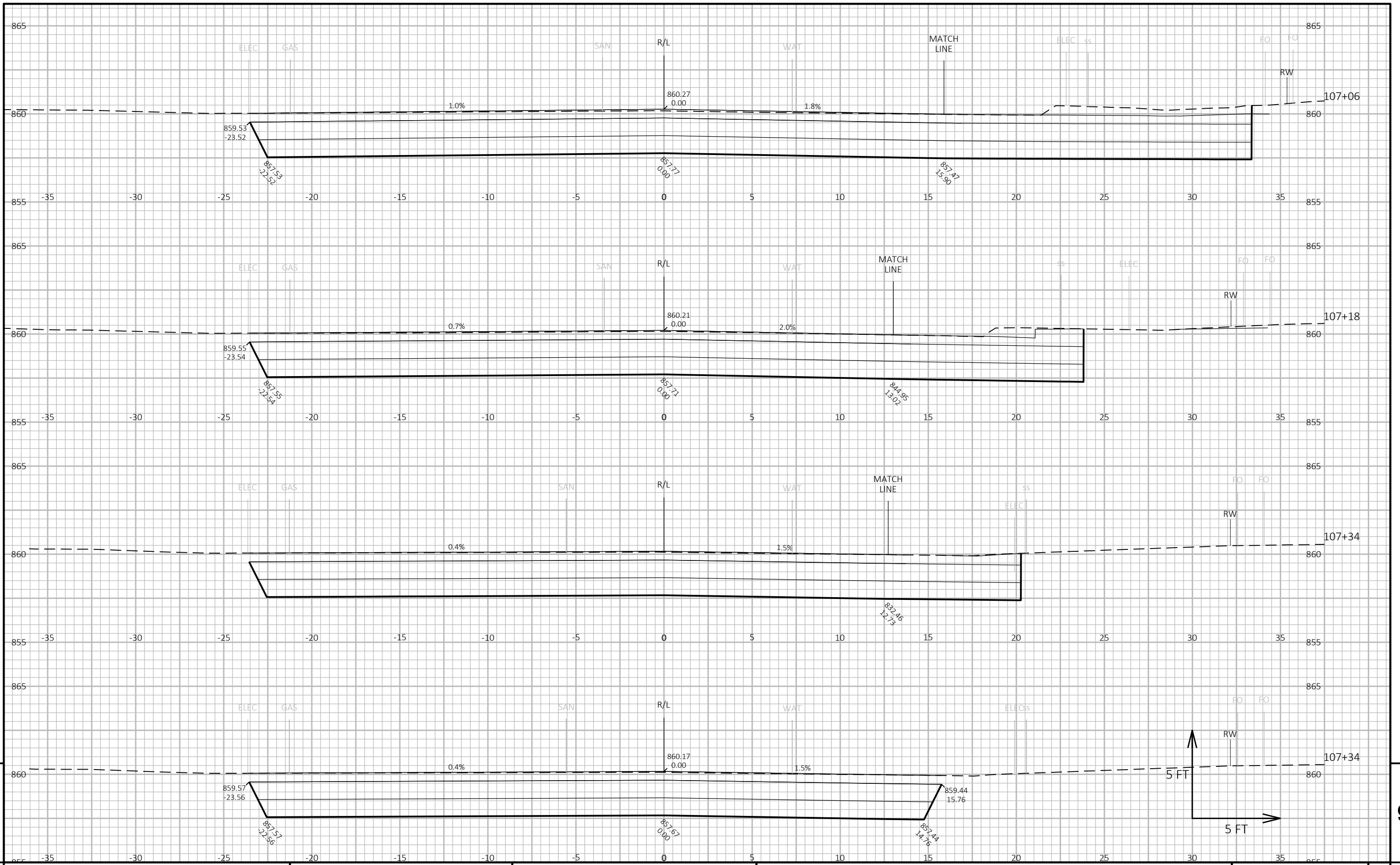
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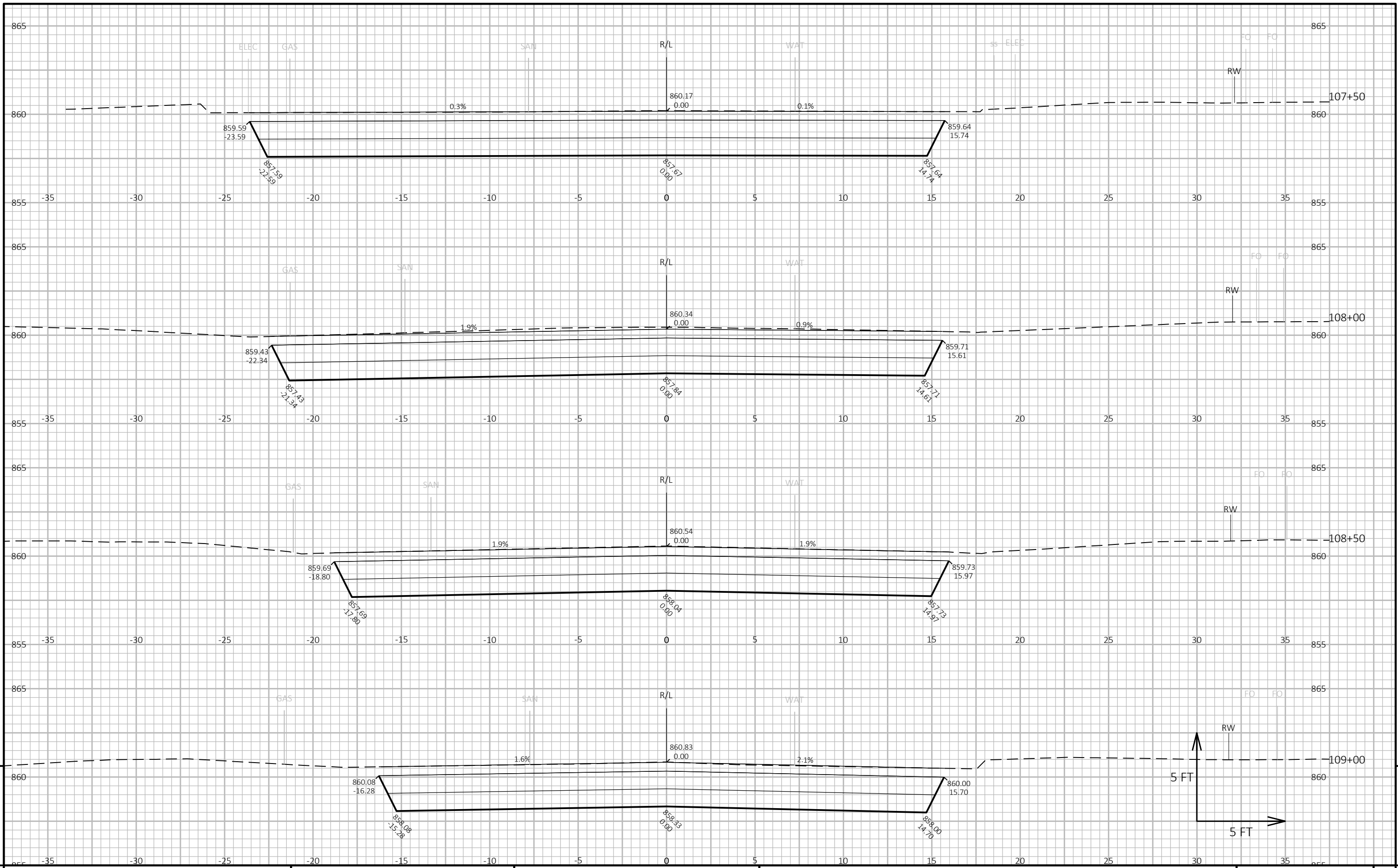
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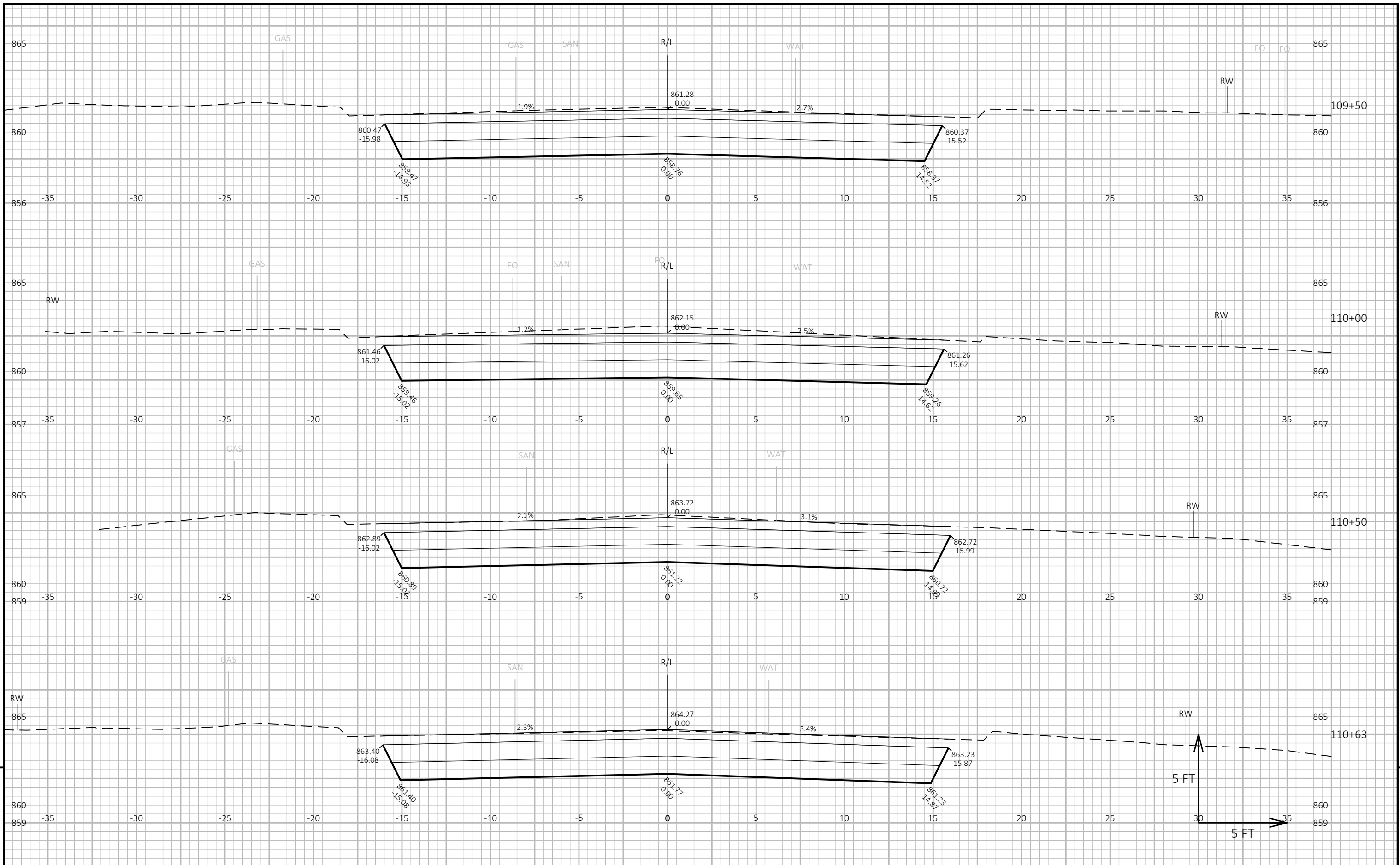
PROJECT NO: 3070-04-60 HWY: STH 73 COUNTY: DANE CROSS SECTIONS: STH 73 SHEET E



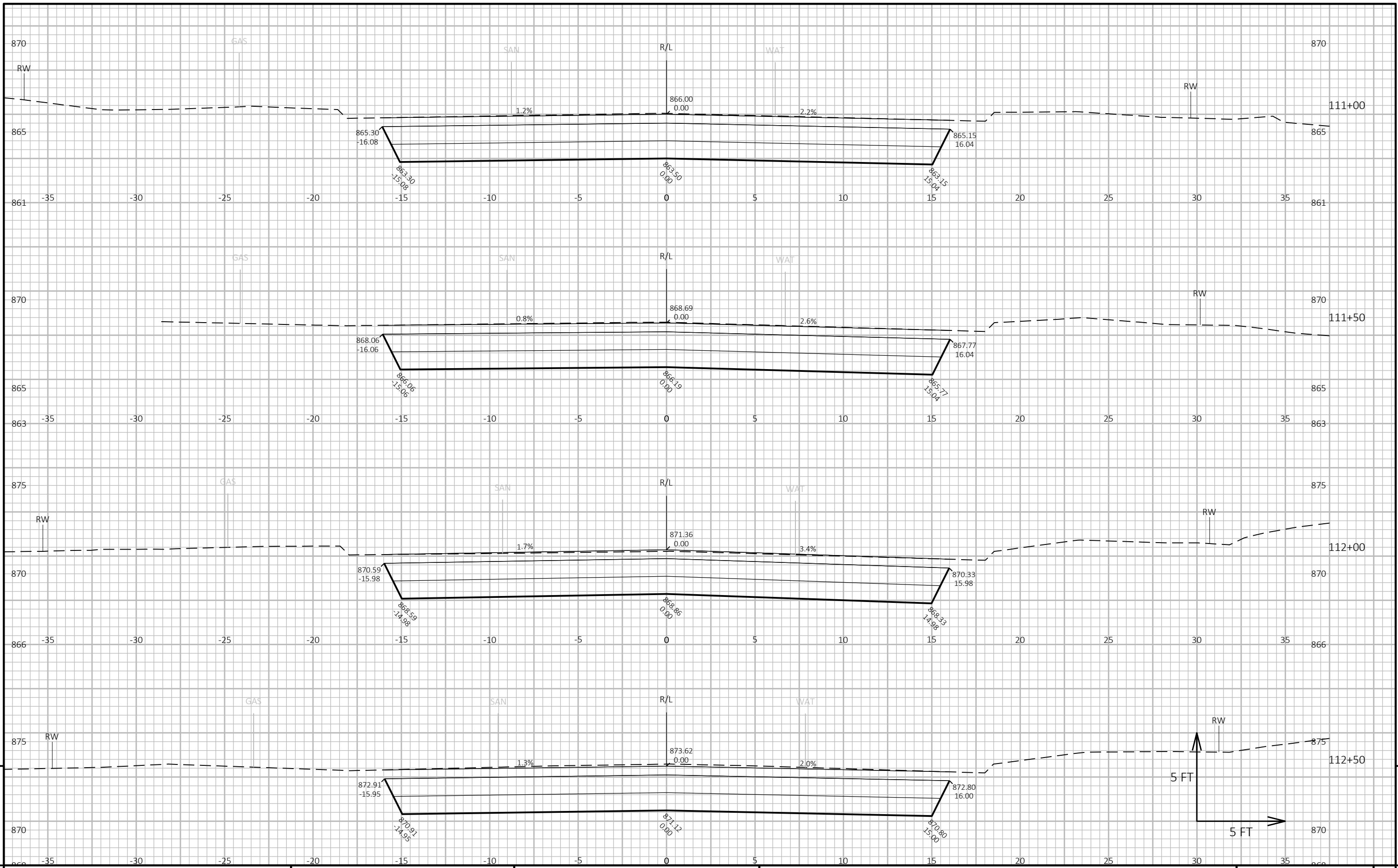
PROJECT NO: 3070-04-60 HWY: STH 73 COUNTY: DANE CROSS SECTIONS: STH 73 SHEET 9



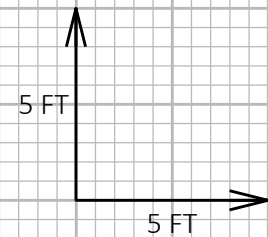
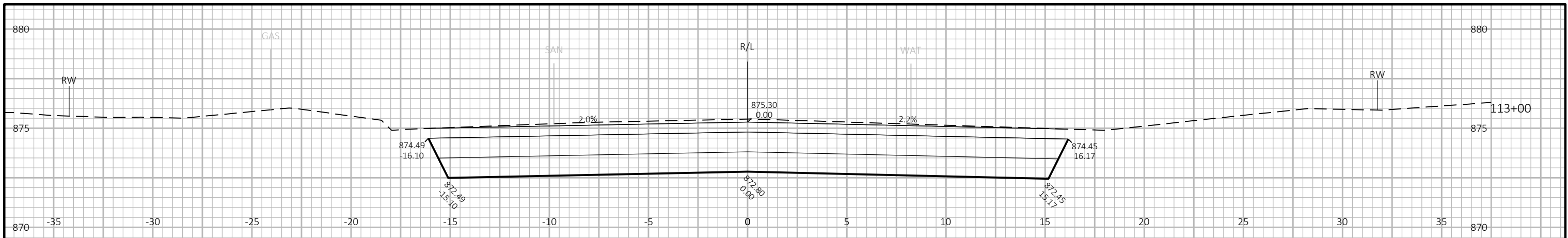
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PROJECT NO: 3070-04-60
HWY: STH 73
COUNTY: DANE
CROSS SECTIONS: STH 73
SHEET
9



PROJECT NO: 3070-04-60 HWY: STH 73 COUNTY: DANE CROSS SECTIONS: STH 73 SHEET E



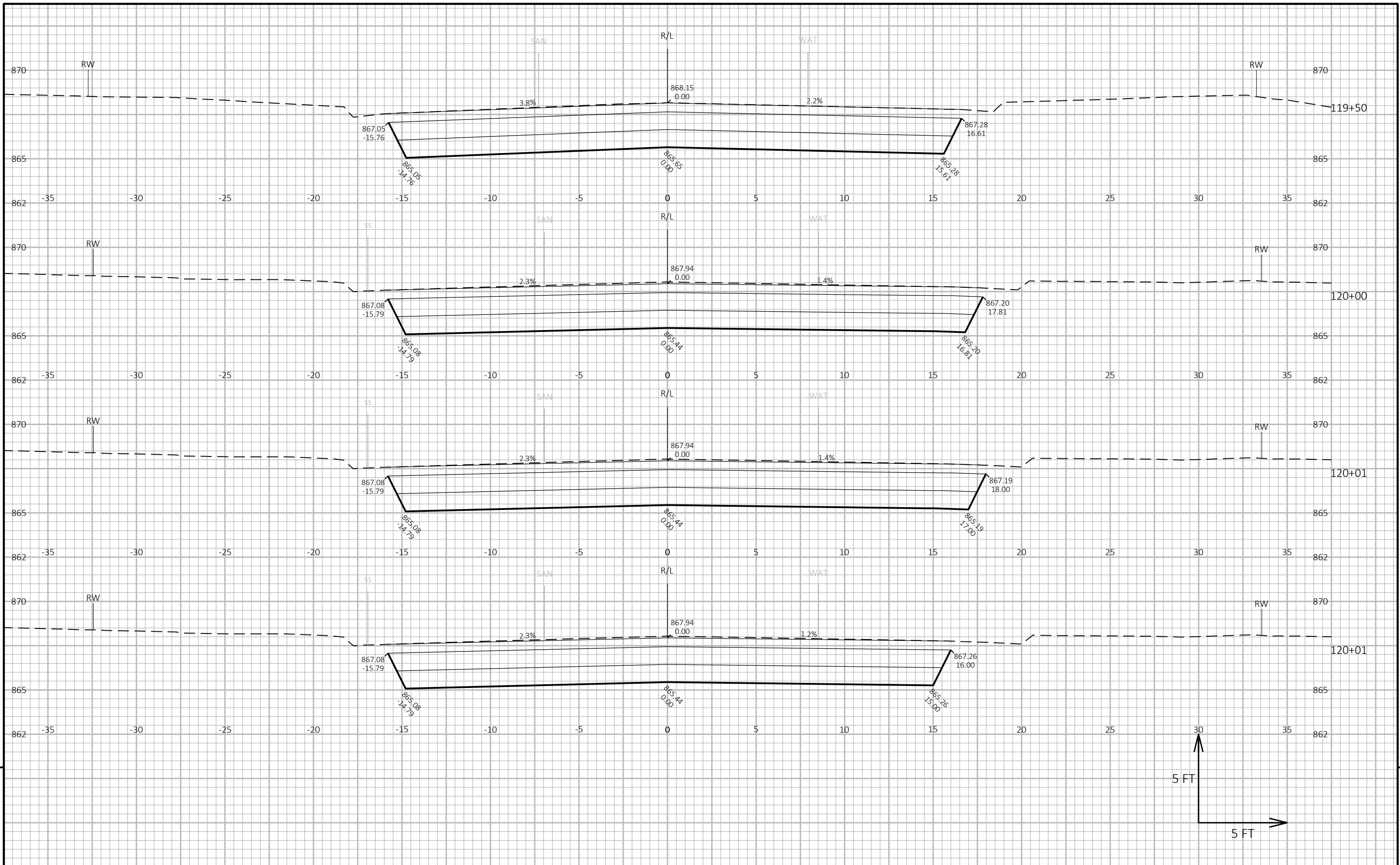
PROJECT NO: 3070-04-60 HWY: STH 73 COUNTY: DANE CROSS SECTIONS: STH 73 SHEET E



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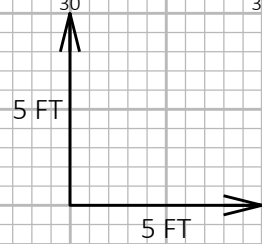
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PROJECT NO: 3070-04-60	HWY: STH 73	COUNTY: DANE	CROSS SECTIONS: STH 73	SHEET	E
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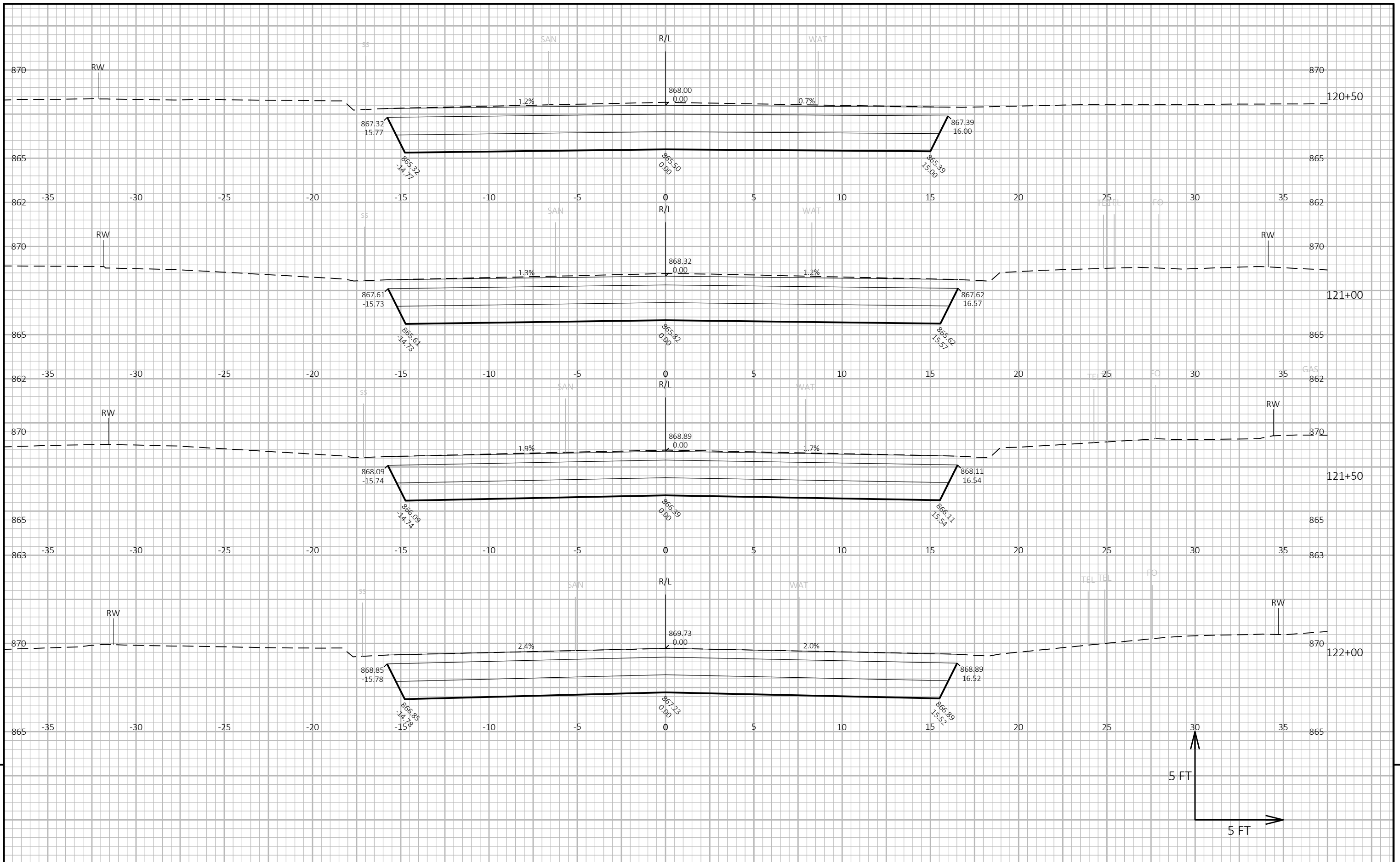
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PROJECT NO: 3070-04-60 HWY: STH 73 COUNTY: DANE CROSS SECTIONS: STH 73 SHEET E

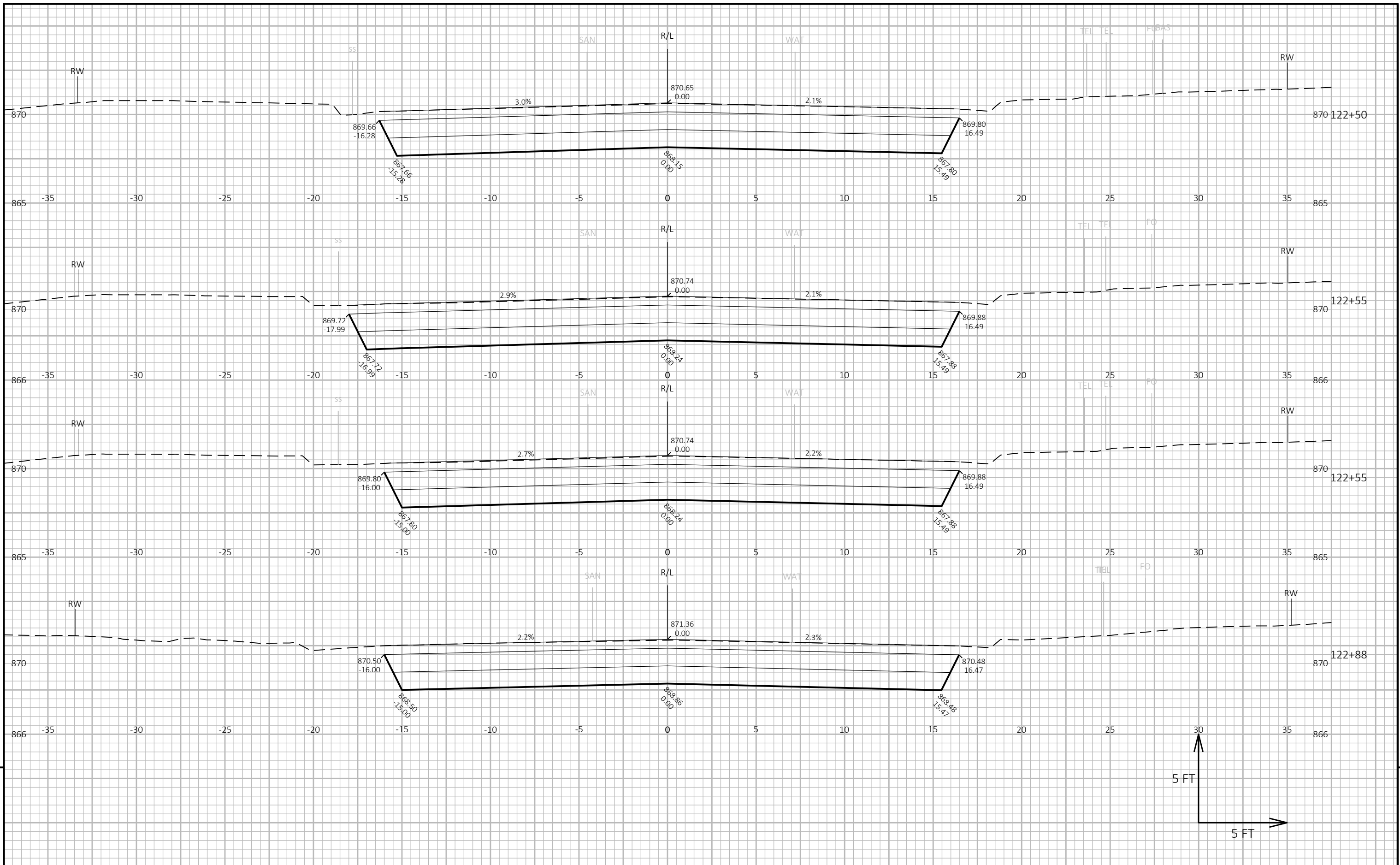
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LAYOUT NAME - 090214-xs



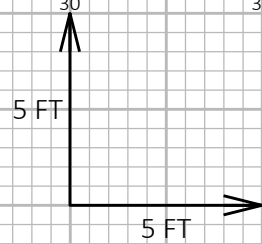
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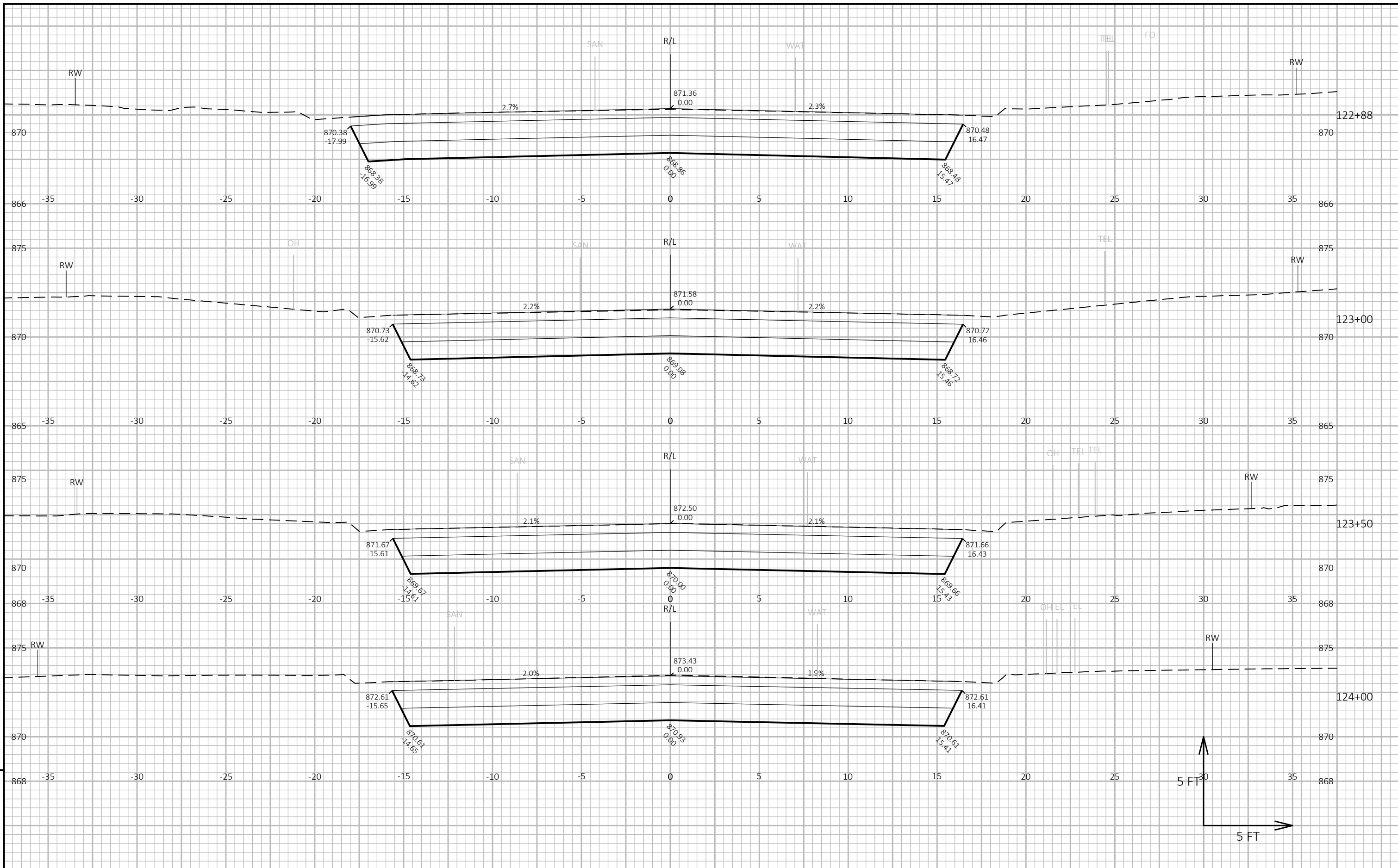
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PROJECT NO: 3070-04-60 HWY: STH 73 COUNTY: DANE CROSS SECTIONS: STH 73 SHEET E

FILE NAME: Y:\50XX\5085_DP.STH73.DAN\CADD\30700460\090201-XS.DWG PLOT DATE: 7/29/2022 3:13 PM PLOT BY: HARRY TRAN PLOT NAME: PLOT SCALE: 1 IN=5 FT HORZ. / 1 IN=5 FT VERT. WISDOT/CADD SHEET 49



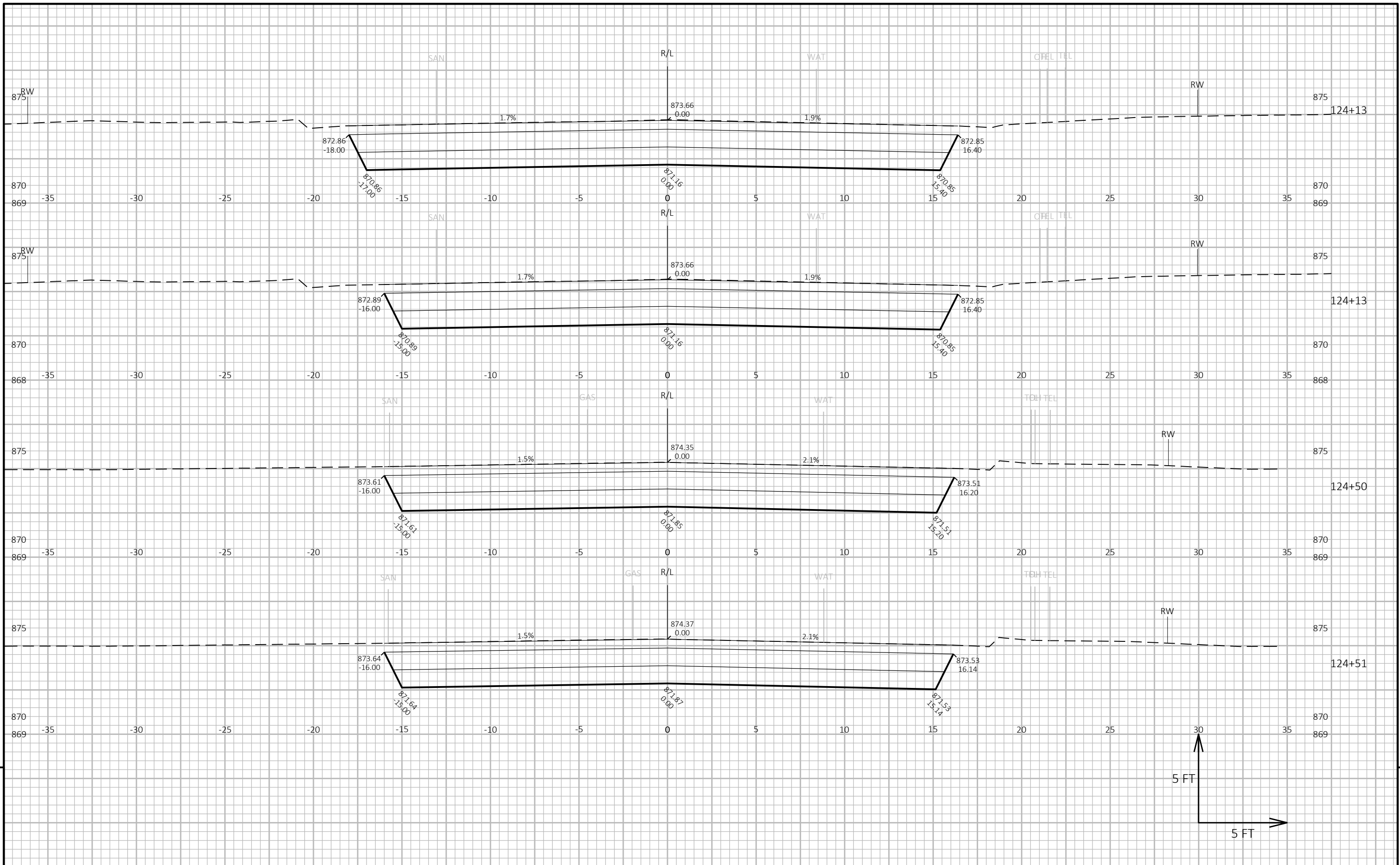
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PROJECT NO: 3070-04-60 HWY: STH 73 COUNTY: DANE CROSS SECTIONS: STH 73 SHEET E

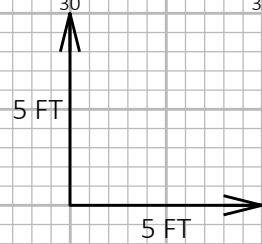
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LAYOUT NAME - 090217-xs

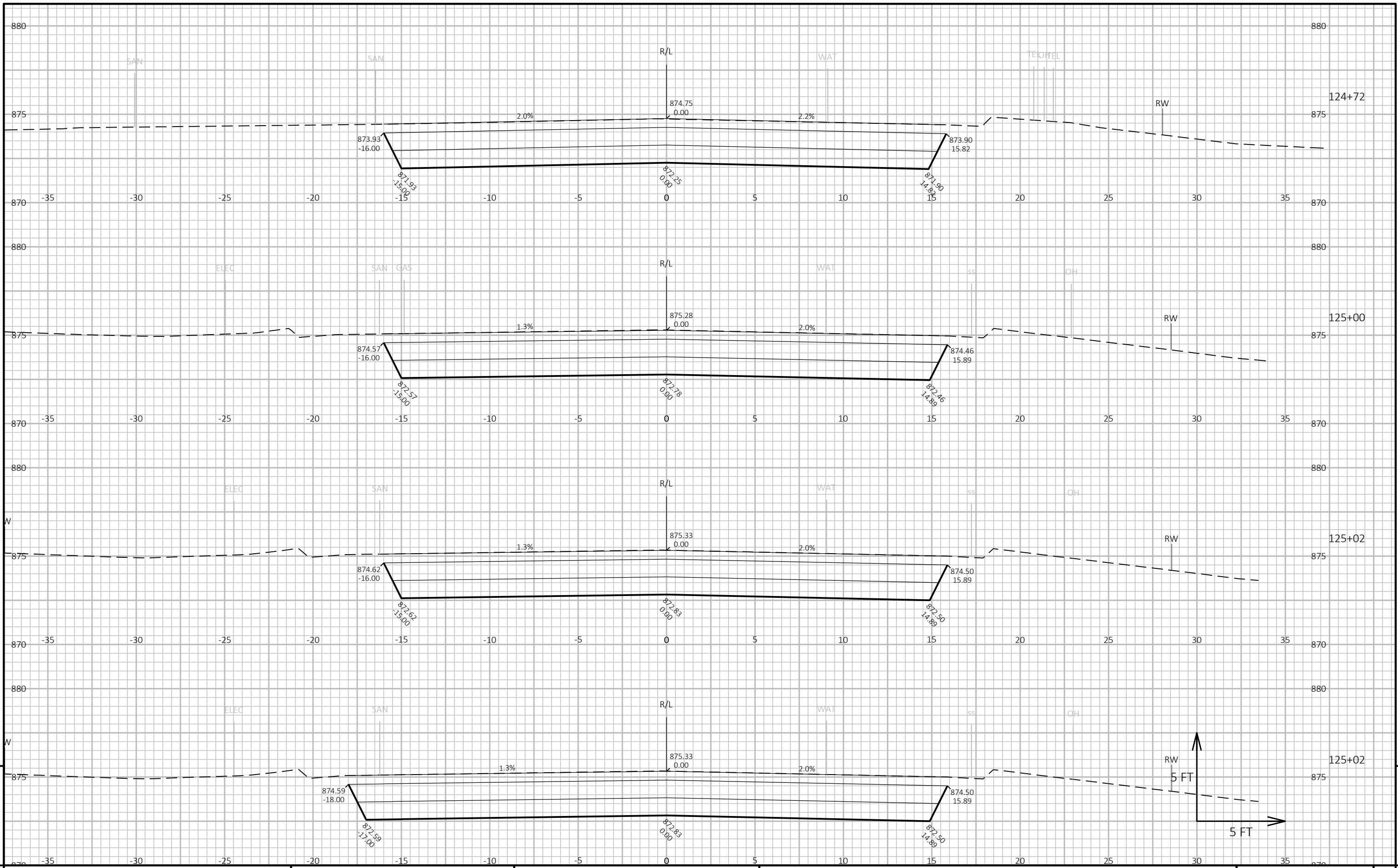


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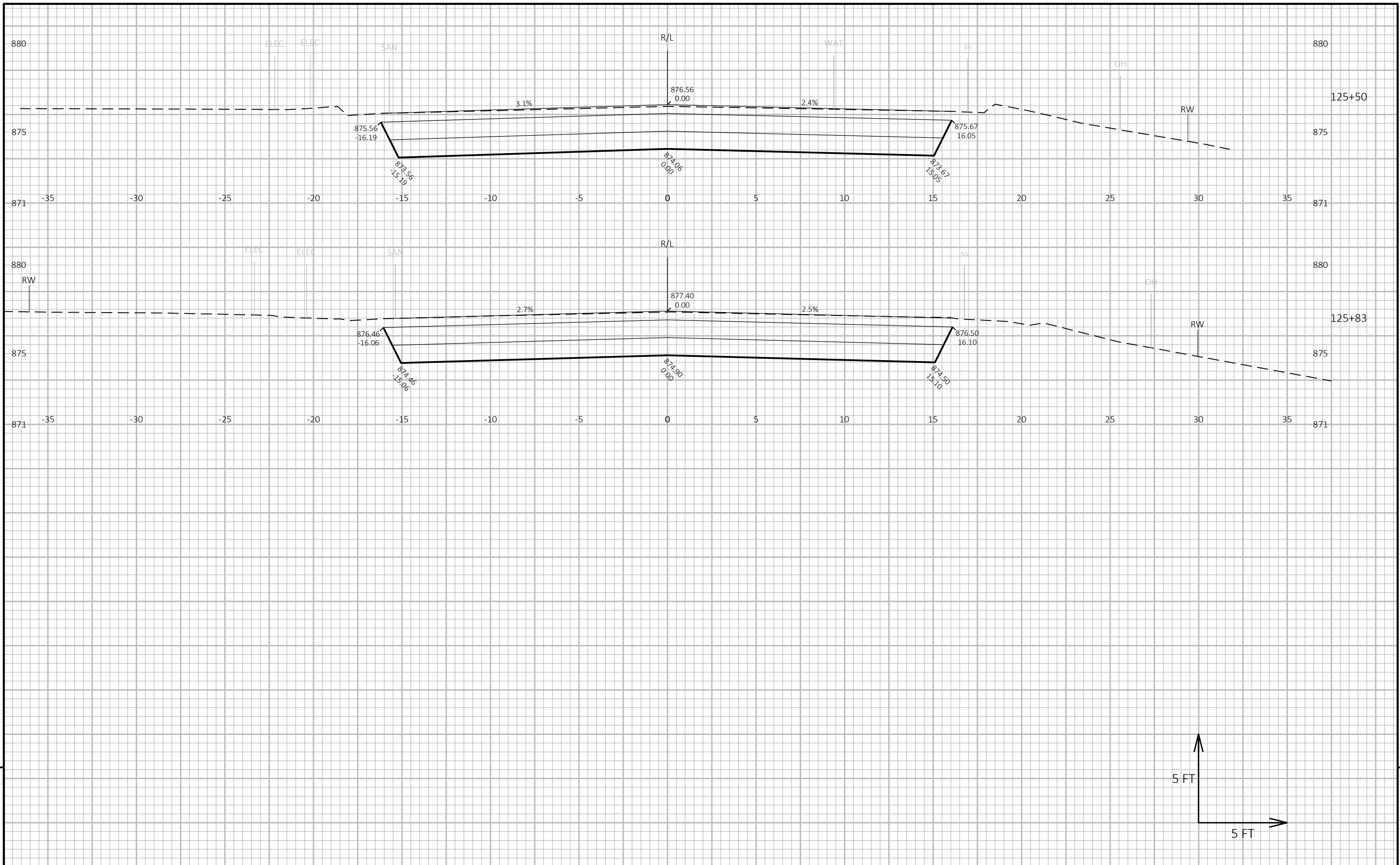
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PROJECT NO: 3070-04-60 HWY: STH 73 COUNTY: DANE CROSS SECTIONS: STH 73 SHEET E

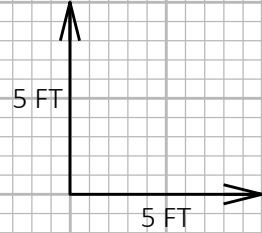


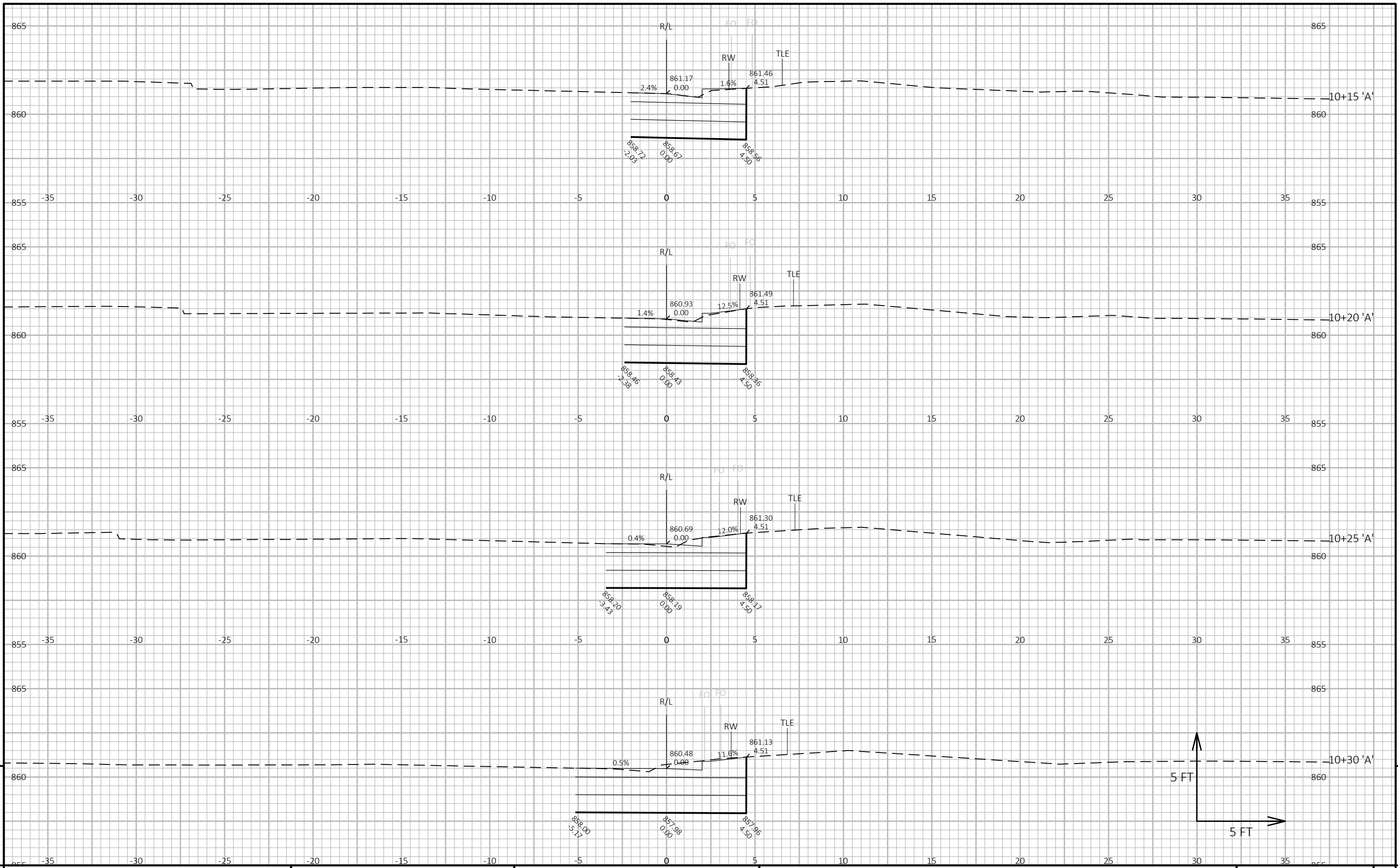
PROJECT NO: 3070-04-60 HWY: STH 73 COUNTY: DANE CROSS SECTIONS: STH 73 SHEET 9



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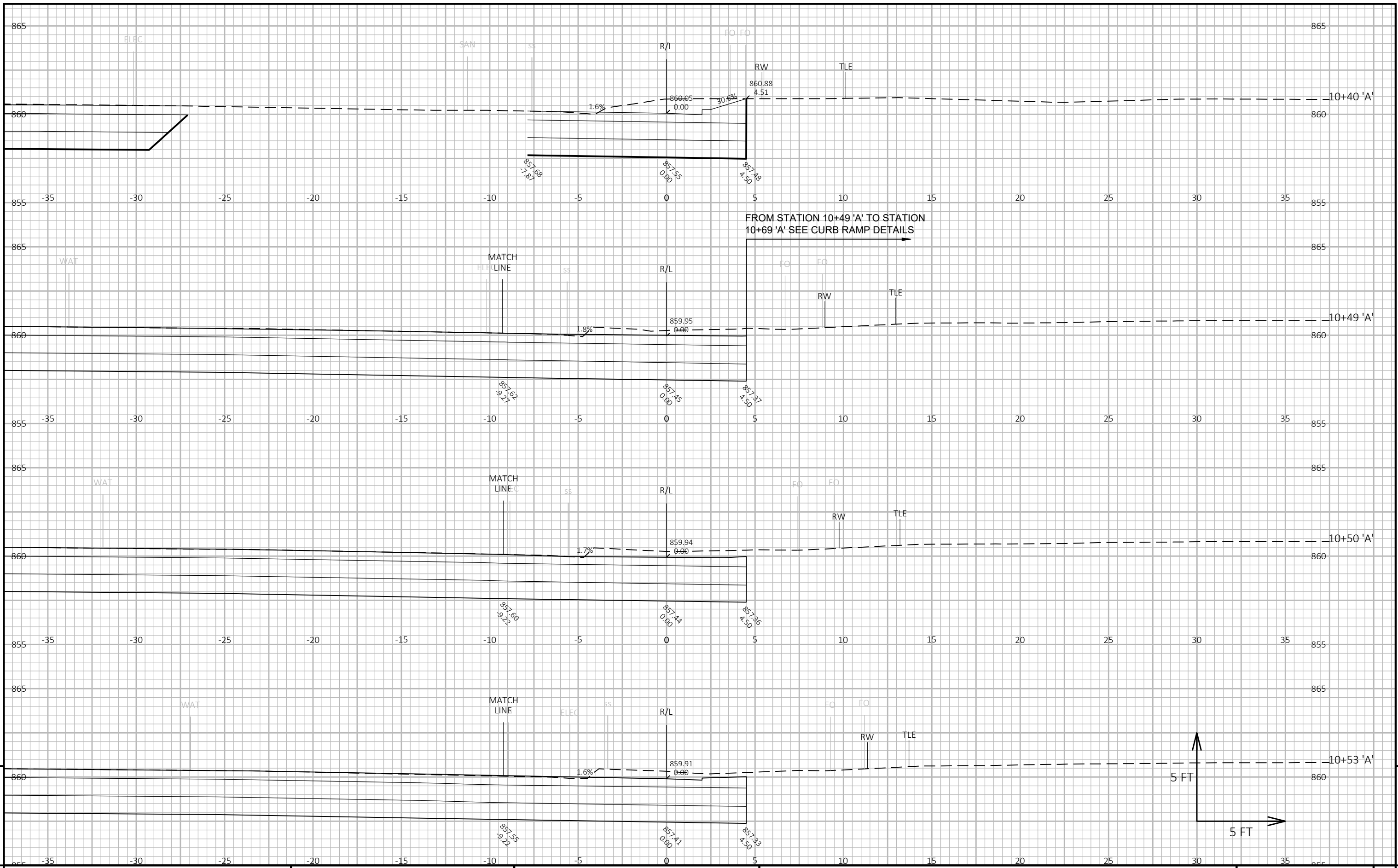
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PROJECT NO: 3070-04-60 HWY: STH 73 COUNTY: DANE CROSS SECTIONS: STH 73 SHEET E

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LAYOUT NAME - 090221-xs



PROJECT NO: 3070-04-60

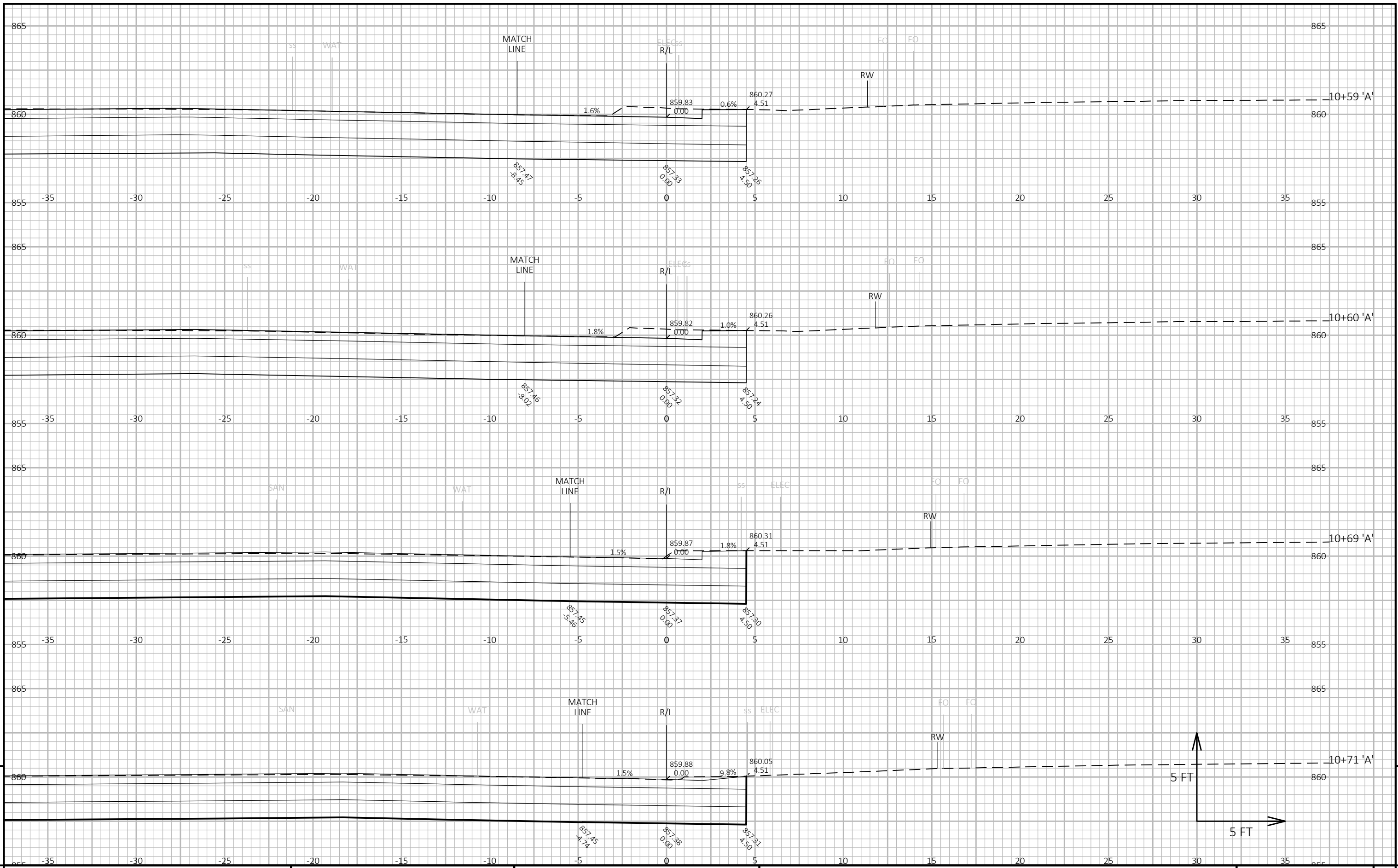
HWY: STH 73

COUNTY: DANE

CROSS SECTIONS: SOUTH INDUSTRIAL PARK ROAD - CURB RETURN

SHEET

E

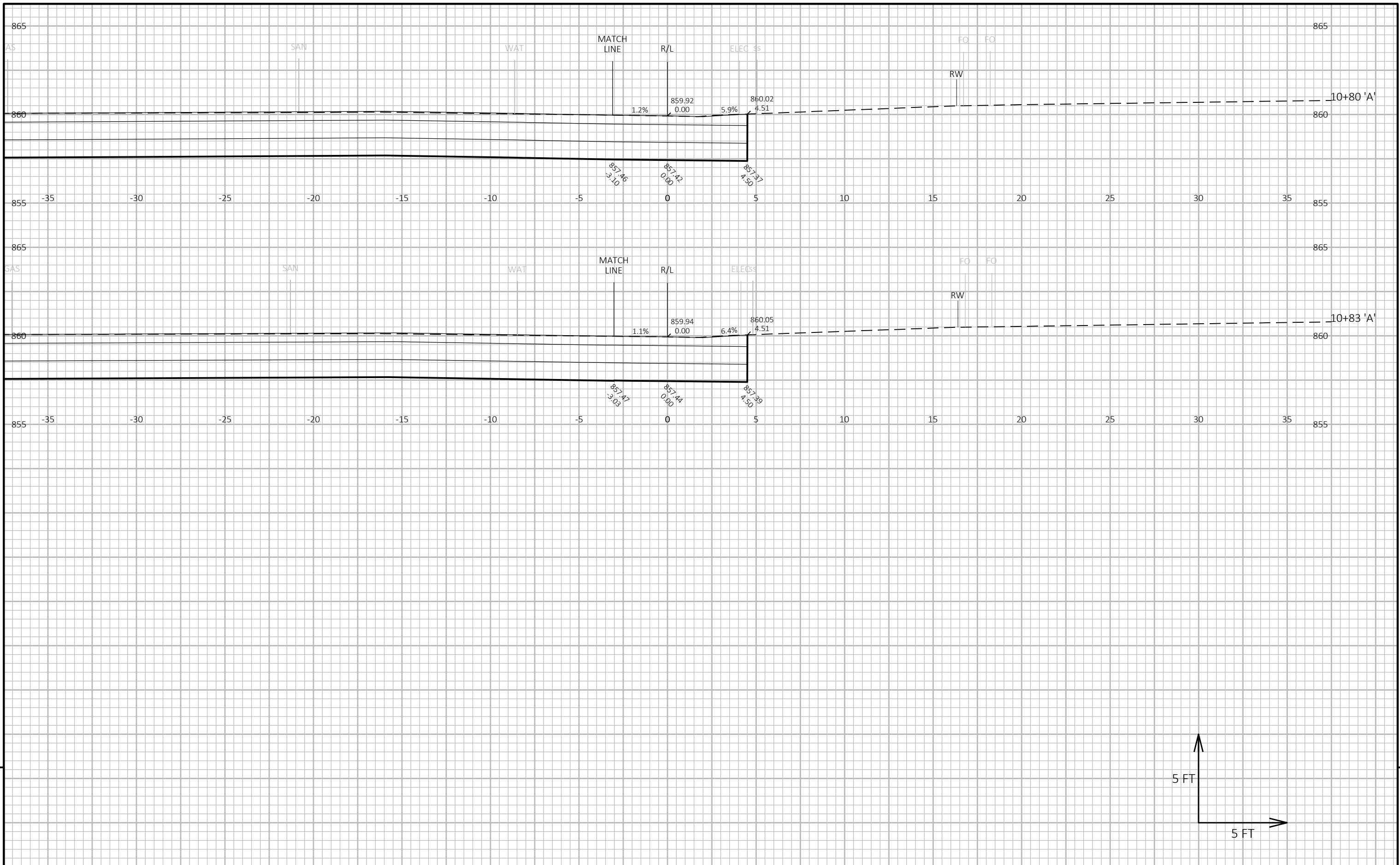


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PROJECT NO: 3070-04-60	HWY: STH 73	COUNTY: DANE	CROSS SECTIONS: SOUTH INDUSTRIAL PARK ROAD - CURB RETURN	SHEET	E
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FILE NAME : Y:\50XX\5085_DP.STH73.DAN\CADD\30700460\090201-XS.DWG PLOT DATE : 7/29/2022 3:14 PM PLOT BY : HARRY TRAN PLOT NAME : LAYOUT NAME - 090223-xs PLOT SCALE : 1 IN:5 FT HORZ. / 1 IN:5 FT VERT. WISDOT/CADD SHEET 49



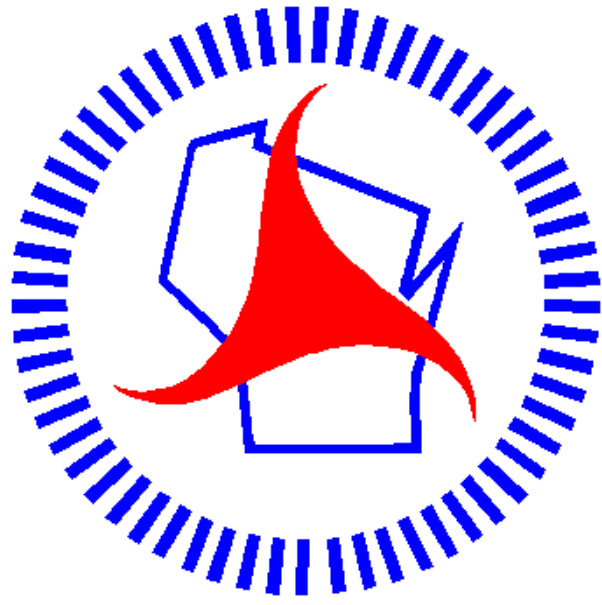
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PROJECT NO: 3070-04-60 HWY: STH 73 COUNTY: DANE CROSS SECTIONS: SOUTH INDUSTRIAL PARK ROAD - CURB RETURN SHEET E

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Notes



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