

MAD
PROJECT ID:
WITH: N/A

5991-02-62

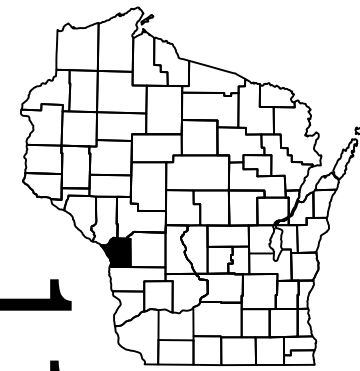
COUNTY:
LA CROSSE

FEBRUARY 2023

ORDER OF SHEETS

| | | |
|-------------|---|------------------------------|
| Section No. | 1 | Title |
| Section No. | 2 | Typical Sections and Details |
| Section No. | 3 | Estimate of Quantities |
| Section No. | 3 | Miscellaneous Quantities |
| Section No. | 4 | Right of Way Plat |
| Section No. | 5 | Plan and Profile |
| Section No. | 6 | Standard Detail Drawings |
| Section No. | 7 | Sign Plates |
| Section No. | 8 | Structure Plans |
| Section No. | 9 | Computer Earthwork Data |
| Section No. | 9 | Cross Sections |

TOTAL SHEETS = 136



DESIGN DESIGNATION

| | | | |
|--------------|------|---|---------|
| A.A.D.T. | 2023 | = | 3,900 |
| A.A.D.T. | 2043 | = | 4,700 |
| D.H.V. | | = | 2.8 |
| D.D. | | = | 59/41 |
| T. | | = | 3.3% |
| DESIGN SPEED | | = | 30 MPH |
| ESALS | | = | 481,800 |

CONVENTIONAL SYMBOLS

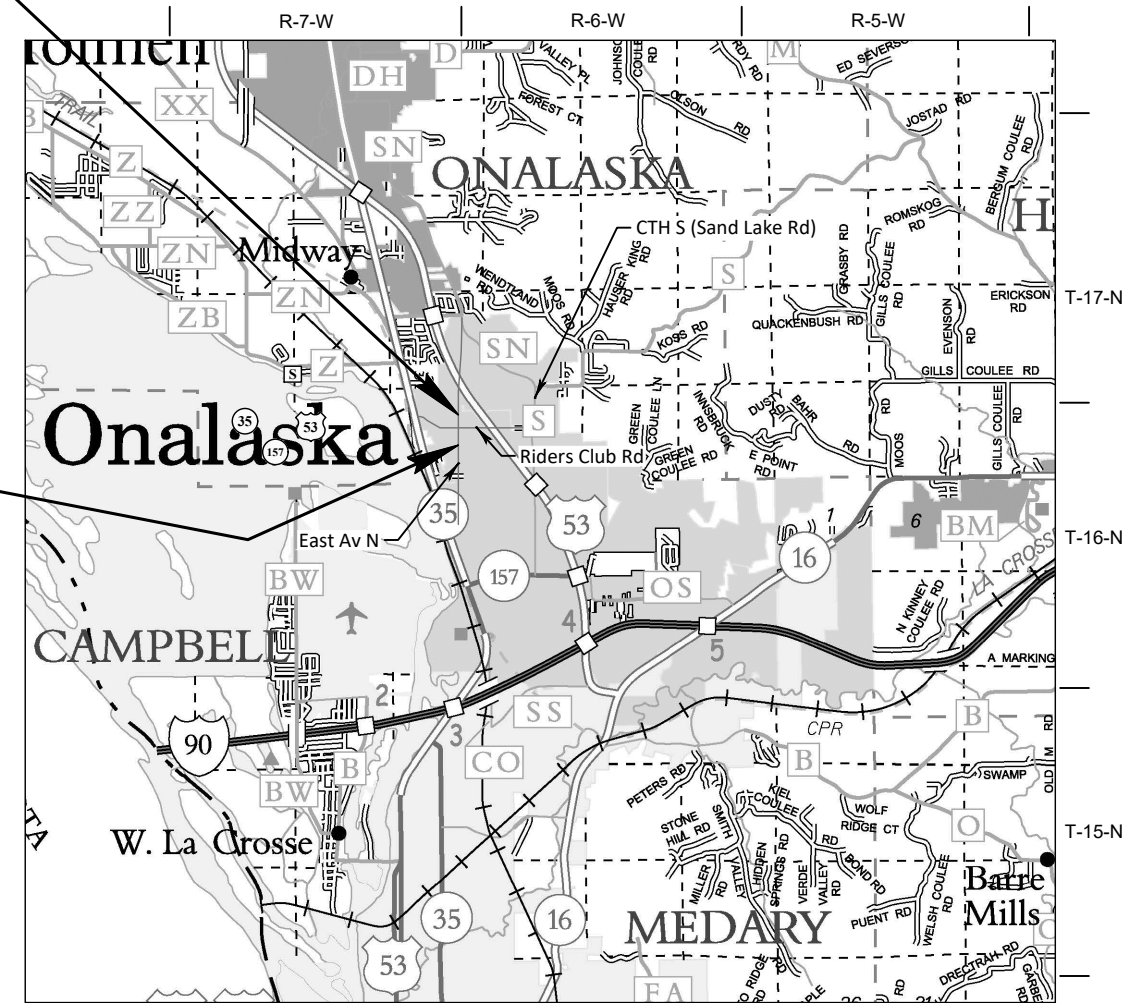
| PLAN | PROFILE |
|-----------------------------------|--|
| CORPORATE LIMITS | GRADE LINE |
| PROPERTY LINE | ORIGINAL GROUND |
| LOT LINE | MARSH OR ROCK PROFILE (To be noted as such) |
| LIMITED HIGHWAY EASEMENT | SPECIAL DITCH |
| EXISTING RIGHT OF WAY | GRADE ELEVATION |
| PROPOSED OR NEW R/W LINE | CULVERT (Profile View) |
| SLOPE INTERCEPT | UTILITIES |
| REFERENCE LINE | ELECTRIC |
| EXISTING CULVERT | FIBER OPTIC |
| PROPOSED CULVERT (Box or Pipe) | GAS |
| COMBUSTIBLE FLUIDS | SANITARY SEWER |
| MARSH AREA | STORM SEWER |
| WOODED OR SHRUB AREA | TELEPHONE |
| | WATER |
| | UTILITY PEDESTAL |
| | POWER POLE |
| | TELEPHONE POLE |

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
PLAN OF PROPOSED IMPROVEMENT
C ONALASKA, EAST AVENUE N
RIDERS CLUB ROAD INTERSECTION
LOC STR
LA CROSSE COUNTY

STATE PROJECT NUMBER
5991-02-62

END PROJECT
STA 502+90.16 'EC'
X = 449494.5412
Y = 165781.9597

BEGIN PROJECT
STA 500+13.75 'EA'
X = 449496.0371
Y = 165505.8704



LAYOUT
SCALE 0 2 MI
TOTAL NET LENGTH OF CENTERLINE = 0.052 MI

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS), LA CROSSE COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES. ELEVATIONS ARE REFERENCED TO NAVD 88 (2012). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 12A.

| STATE PROJECT | FEDERAL PROJECT | |
|---------------|-----------------|----------|
| | PROJECT | CONTRACT |
| 5991-02-62 | WISC 2023255 | 1 |
| | | |
| | | |

ACCEPTED FOR
CITY OF ONALASKA
Date: 10-11-22 C. J. Leonard, O.P.W.
(Signature and Title of Official)

ORIGINAL PLANS PREPARED BY

Building a Better World for All of Us

DATE: 10/10/2022
(Professional Engineer Signature)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

| | |
|---------------------|----------------|
| PREPARED BY | |
| Surveyor | SEH |
| Designer | SEH |
| Project Manager | BRANDAN BURGER |
| Regional Examiner | SW REGION |
| Regional Supervisor | KYLE HEMP |

APPROVED FOR THE DEPARTMENT
DATE: 10/11/2022 Brandan Burger
(Signature)

STANDARD ABBREVIATIONS

| | | | |
|--------|--|---------|--------------------------------------|
| ABUT | ABUTMENT | ID | INSIDE DIAMETER |
| AC | ACRE | INV | INVERT |
| AGG | AGGREGATE | IP | IRON PIPE ON PIN |
| AECPRC | APRON ENDWALL FOR CULVERT PIPE REINFORCED CONCRETE | LHF | LEFT-HAND FORWARD |
| AECPCS | APRON ENDWALL FOR CULVERT PIPE CORRUGATED STEEL | L | LENGTH OF CURVE |
| ASPH | ASPHALTIC | LF | LINEAR FOOT |
| AVG | AVERAGE | LC | LONG CHORD OF CURVE |
| ADT | AVERAGE DAILY TRAFFIC | LS | LUMP SUM |
| BF | BACK FACE | MH | MANHOLE |
| BM | BENCH MARK | MOR | MID POINT OF RADIUS |
| BR | BRIDGE | NC | NORMAL CROWN |
| CE | COMMERCIAL ENTRANCE | NO | NUMBER |
| C/L | CENTER LINE | OBLIT | OBLITERATE |
| Δ | CENTRAL ANGLE OR DELTA | PAVT | PAVEMENT |
| COB | CENTER OF BARRIER | PE | PRIVATE ENTRANCE |
| CONC | CONCRETE | PVRC | POINT OF VERTICAL REVERSE CURVE |
| CPRC | CULVERT PIPE REINFORCED CONCRETE | QOR | QUARTER POINT OF RADIUS |
| CPRCHE | CULVERT PIPE REINFORCED CONCRETE HORIZONTAL ELLIPTICAL | R | RADIUS |
| CR | CREEK | REQ'D | REQUIRED |
| CY | CUBIC YARD | RES | RESIDENCE OR RESIDENTIAL |
| C&G | CURB AND GUTTER | RHF | RIGHT-HAND FORWARD |
| D | DEGREE OF CURVE | R/W | RIGHT-OF-WAY |
| DHV | DESIGN HOUR VOLUME | R | RIVER |
| DISCH | DISCHARGE | RDWY | ROADWAY |
| DG | DITCH GRADE | R/L | REFERENCE LINE |
| DWY | DRIVEWAY | SALV | SALVAGED |
| X | EAST GRID COORDINATE | SAN | SANITARY SEWER |
| EAT | STEEL PLATE BEAM GUARD ENERGY ABSORBING TERMINAL | SF | SQUARE FEET |
| EOR | END POINT OF RADIUS | SY | SQUARE YARD |
| EL | ELEVATION | SDD | STANDARD DETAIL DRAWINGS |
| ENT | ENTRANCE | STA | STATION |
| ESALS | EQUIVALENT SINGLE AXLE LOADS | SS | STORM SEWER |
| EXC | EXCAVATION | SSPRC | STORM SEWER PIPE REINFORCED CONCRETE |
| EBS | EXCAVATION BELOW SUBGRADE | SE | SUPERELEVATION RATE |
| EXIST | EXISTING | TC | TOP OF CURB |
| FC | FACE OF CURB | T OR TN | TOWN |
| FF | FACE TO FACE | T | TRUCKS (PERCENT OF) |
| FERT | FERTILIZE | TYP | TYPICAL |
| FE | FIELD ENTRANCE | VAR | VARIABLE |
| FL | FLOW LINE | VC | VERTICAL CURVE |
| FO | FIBER OPTIC | Y | NORTH GRID COORDINATE |
| CWT | HUNDREDWEIGHT | YD | YARD |
| HYD | HYDRANT | | |

RUNOFF COEFFICIENT TABLE

| | HYDROLOGIC SOIL GROUP | | | | | | | | | | | |
|-------------------------|-----------------------|-----|----------|-----------------------|-----|----------|-----------------------|-----|----------|----------------------|-----|----------|
| | A | | | B | | | C | | | D | | |
| | SLOPE RANGE (PERCENT) | | | SLOPE RANGE (PERCENT) | | | SLOPE RANGE (PERCENT) | | | SLOPERANGE (PERCENT) | | |
| LAND USE: | 0-2 | 2-6 | 6 & OVER | 0-2 | 2-6 | 6 & OVER | 0-2 | 2-6 | 6 & OVER | 0-2 | 2-6 | 6 & OVER |
| ROW CROPS | .08 | .16 | .22 | .12 | .20 | .27 | .15 | .24 | .33 | .19 | .28 | .38 |
| | .22 | .30 | .38 | .26 | .34 | .44 | .30 | .37 | .50 | .34 | .41 | .56 |
| MEDIAN STRIP-TURF | .19 | .20 | .24 | .19 | .22 | .26 | .20 | .23 | .30 | .20 | .25 | .30 |
| | .24 | .26 | .30 | .25 | .28 | .33 | .26 | .30 | .37 | .27 | .32 | .40 |
| SIDE SLOPE-TURF | | | .25 | | | .27 | | | .28 | | | .30 |
| | | | .32 | | | .34 | | | .36 | | | .38 |
| PAVEMENT: | | | | | | | | | | | | |
| ASPHALT | .70 - .95 | | | | | | | | | | | |
| CONCRETE | .80 - .95 | | | | | | | | | | | |
| BRICK | .70 - .80 | | | | | | | | | | | |
| DRIVES, WALKS | .75 - .85 | | | | | | | | | | | |
| ROOFS | .75 - .95 | | | | | | | | | | | |
| GRAVEL ROADS, SHOULDERS | .40 - .60 | | | | | | | | | | | |

TOTAL PROJECT AREA = 0.63ACRES
TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = 0.63ACRES

WISDOT CONTACT:

WISCONSIN DEPT OF TRANSPORTATION
SOUTHWEST REGION
2102 WRIGHT ST
MADISON, WI 53704
TELEPHONE: 608.267.4019
ATTENTION: BRANDAN BURGER, P.E.
EMAIL: BRANDAN.BURGER@DOT.WI.GOV

DNR AREA LIAISON:

WI DEPT OF NATURAL RESOURCES
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3550 MORMON COULEE RD
LA CROSSE, WI 54601
TELEPHONE: 608.785.9115
ATTENTION: KAREN KALVELAGE
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PERRY.MCCLELLAN@CHARTERCOM.COM

BRIGHTSPEED - COMMUNICATIONS
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LA CROSSE, WI 54601
608.615.4136 / 608.780.1238 (CELL)
BRIAN.STELPLUGH@LUMEN.COM

DAIRYLAND POWER - TRANSMISSION
MIKE LYDON
608.787.1381
MICHAEL.LYDON@DAIRYLANDPOWER.COM

CITY OF ONALASKA - WATER
JARROD HOLTER
415 MAIN STREET
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CITY OF ONALASKA CONTACT:

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TELEPHONE: 608.781.9537
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DIRECTOR OF PUBLIC WORKS
EMAIL: JHOLTER@ONALASKAWI.GOV

DESIGN CONTACT:

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LA CROSSE, WI 54601
TELEPHONE: 608.498.4019
ATTENTION: TOREY LEONARD, P.E.
PROJECT MANAGER
EMAIL: TLEONARD@SEHINC.COM

CITY OF ONALASKA - FIBER OPTIC
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RIVERLAND ENERGY (TREMPEALEAU ELECTRIC COOPERATIVE) - ELECTRICITY
BILL MASON
1800 GRANARY ST
HOLMEN, WI 54636
608.769.2880 (CELL)
608.323.3381 (OFFICE)
BMASON@RIVERLANDENERGY.COM

XCEL ENERGY - TRANSMISSION
MITCHELL DIENGER
414 NICOLLET MALL, 5TH FLOOR
MINNEAPOLIS, MN 55401
612.321.3109

XCEL ENERGY - NATURAL GAS
ED PRZYTARSKI
3215 COMMERCE STREET
LA CROSSE, WI 54603
608.780.0151
EDWARD.R.PRZYTARSKI@XCELENERGY.COM

ORDER OF SHEETS - SECTION 2:

- GENERAL NOTES
- PROJECT OVERVIEW
- TYPICAL SECTIONS
- CONSTRUCTION DETAILS
- PLAN DETAILS
- INTERSECTION DETAILS
- CURB RAMP DETAILS
- UTILITY PLAN
- PERMANENT SIGNING
- LIGHTING PLAN
- PAVEMENT MARKING
- DETOUR PLANS
- PEDESTRIAN ACCOMMODATIONS
- ALIGNMENT DETAILS
- CONTROL POINTS

GENERAL NOTES:

- NO TREES OR SHRUBS SHALL BE REMOVED WITHOUT APPROVAL OF THE ENGINEER.
- THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.
- THE CONTRACTOR SHALL NOTIFY DIGGERS HOTLINE AND AFFECTED UTILITIES PRIOR TO THE START OF WORK. ANY LOCAL MUNICIPAL UTILITY WHICH IS NOT A MEMBER OF THE DIGGERS HOTLINE MUST BE CONTACTED SEPARATELY.
- CROSS SECTIONS SHOWN INCLUDE THE THICKNESS OF TOPSOIL WHERE REQUIRED. TOPSOIL SHALL BE REPLACED WITH 4-INCH TYPICAL DEPTH.
- TRAFFIC CONTROL DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.
- ASPHALTIC AND CONCRETE SURFACES SHALL BE SAWCUT AT THE MATCH LINE AS SHOWN ON THE PLAN OR AS DIRECTED BY THE ENGINEER.
- ALL DISTURBED AREAS SHALL BE RESTORED WITH SOD LAWN.
- A CONVERSION FACTOR OF 2.0 TONS/CY IS USED TO ESTIMATE QUANTITIES FOR BASE AGGREGATE DENSE.
- APPLY TACK COAT AT A RATE OF 0.05 GA/SY BETWEEN LAYERS OF HMA PAVEMENT.
- HMA PAVEMENT WEIGHT CALCULATIONS ARE BASED ON 112 LB/SY/IN.
- THE CONTRACTOR'S PAVING OPERATIONS SHALL BE CONSISTENT WITH THE PLAN AND TYPICAL SECTIONS AND CONSTRUCTED TO PREVENT HMA LONGITUDINAL JOINTS FROM BEING LOCATED WITHIN A DRIVING, TURNING, PASSING OR PARKING LANE.
- ALL TYPES OF ASPHALTIC PAVEMENT SHALL BE CONSTRUCTED WITH THE FOLLOWING LAYERS AND GRADATIONS:
 - 2.5" HMA PAVEMENT 4 LT 58-28 S (UPPER LAYER)
 - 2.5" HMA PAVEMENT 3 LT 58-28 S (LOWER LAYER)



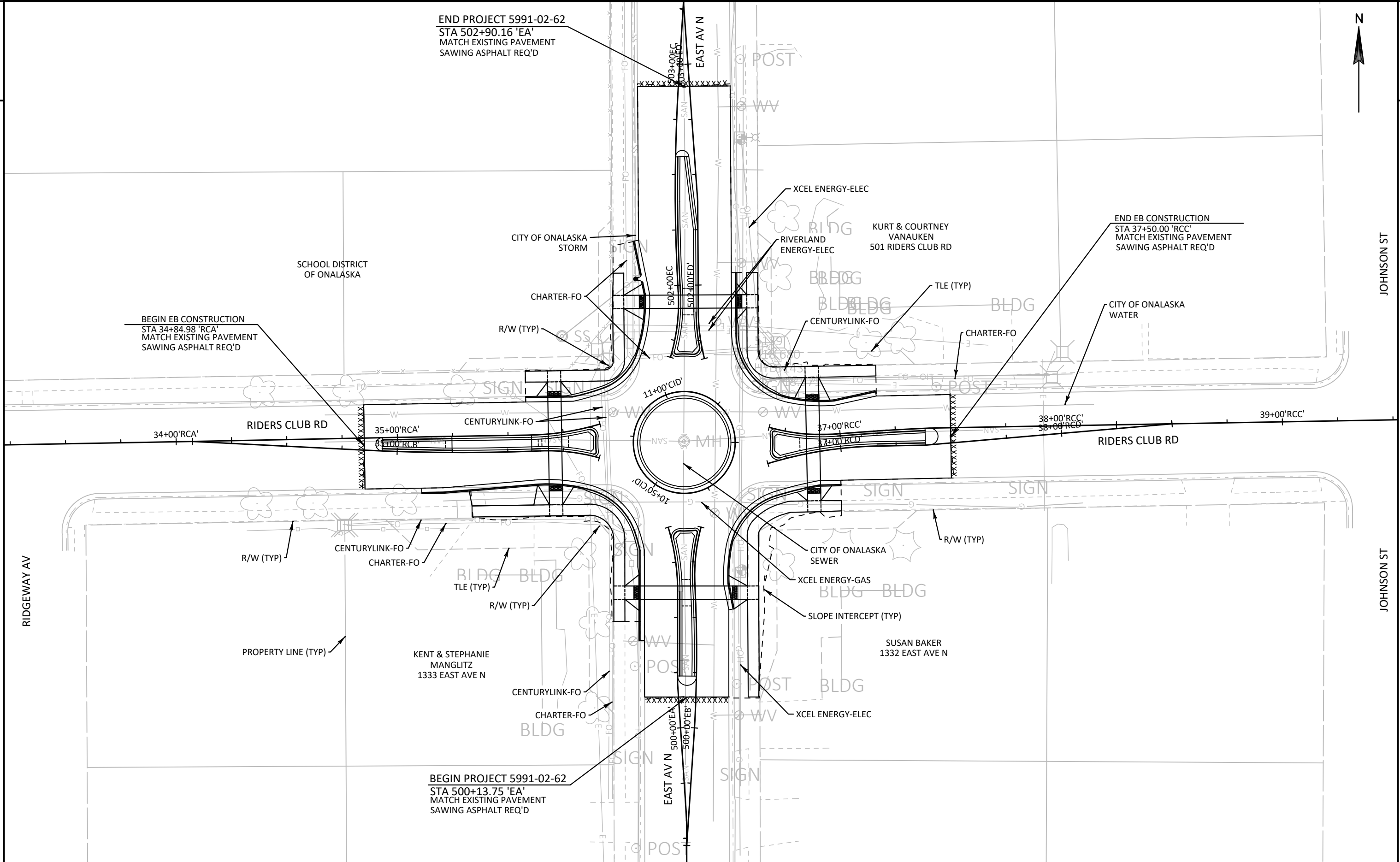


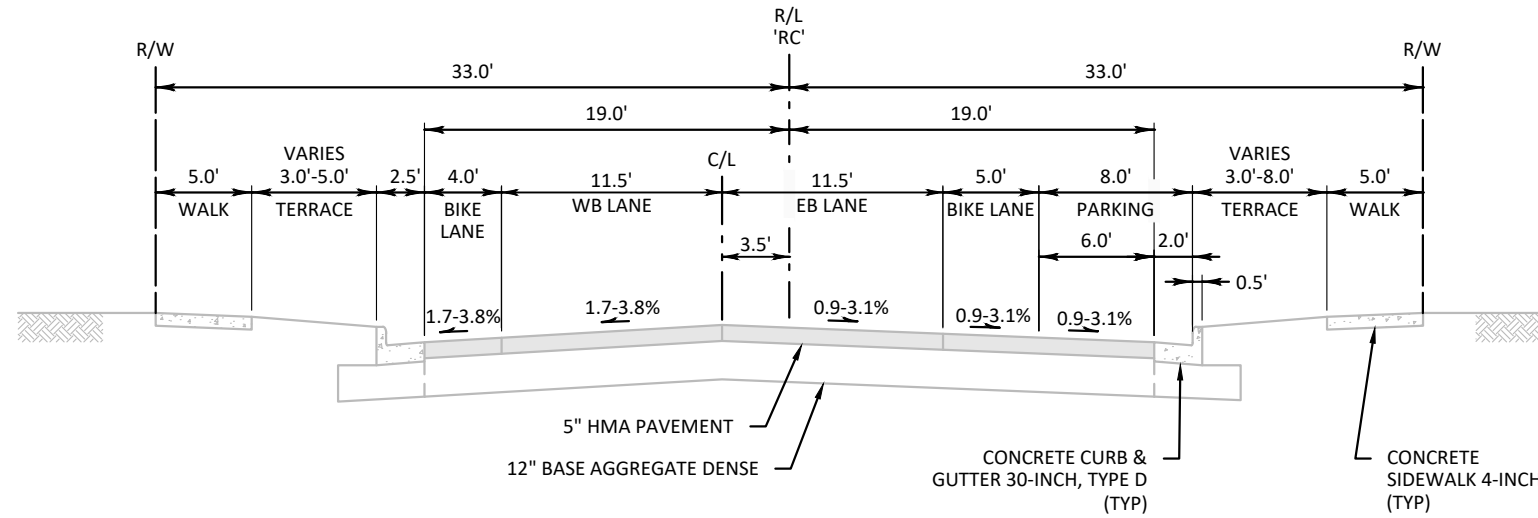
END PROJECT 5991-02-62
STA 502+90.16 'EA'
MATCH EXISTING PAVEMENT
SAWING ASPHALT REQ'D

BEGIN EB CONSTRUCTION
STA 34+84.98 'RCA'
MATCH EXISTING PAVEMENT
SAWING ASPHALT REQ'D

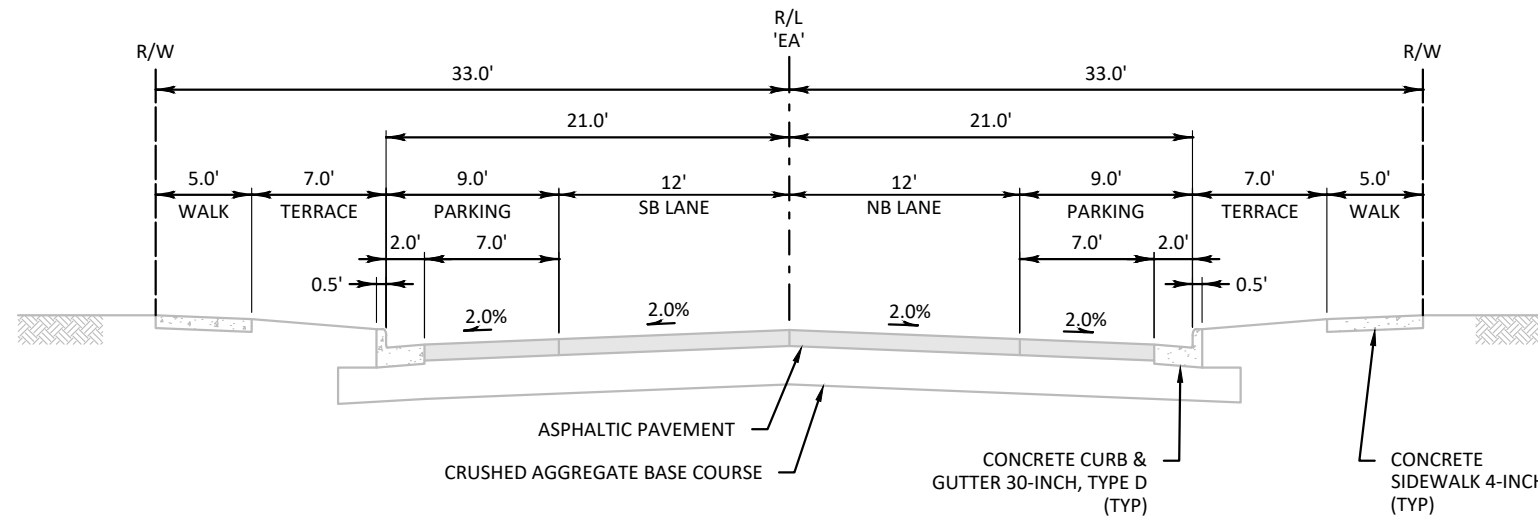
END EB CONSTRUCTION
STA 37+50.00 'RCC'
MATCH EXISTING PAVEMENT
SAWING ASPHALT REQ'D

BEGIN PROJECT 5991-02-62
STA 500+13.75 'EA'
MATCH EXISTING PAVEMENT
SAWING ASPHALT REQ'D





EXISTING TYPICAL SECTION - RIDERS CLUB ROAD
 STA 34+84 RCA - STA 37+50 RCA

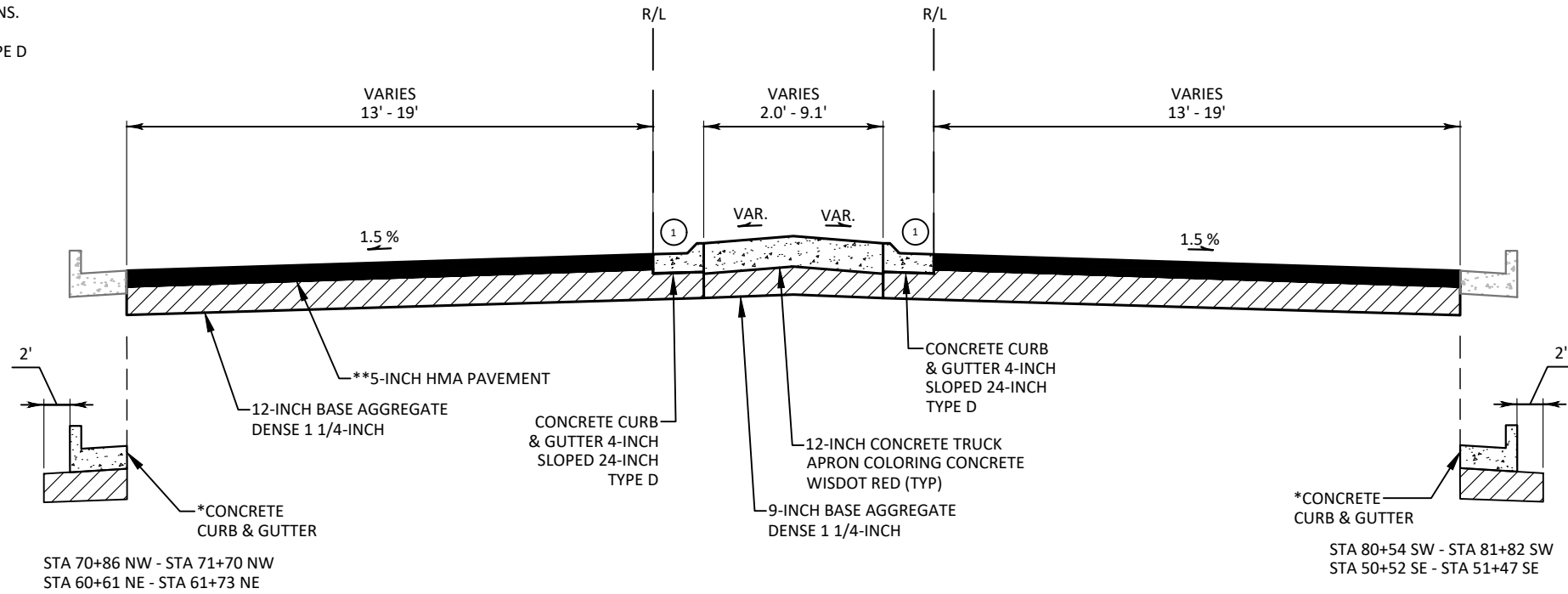


EXISTING TYPICAL SECTION - EAST AVENUE N
 STA 500+13 EA - STA 502+90 EA

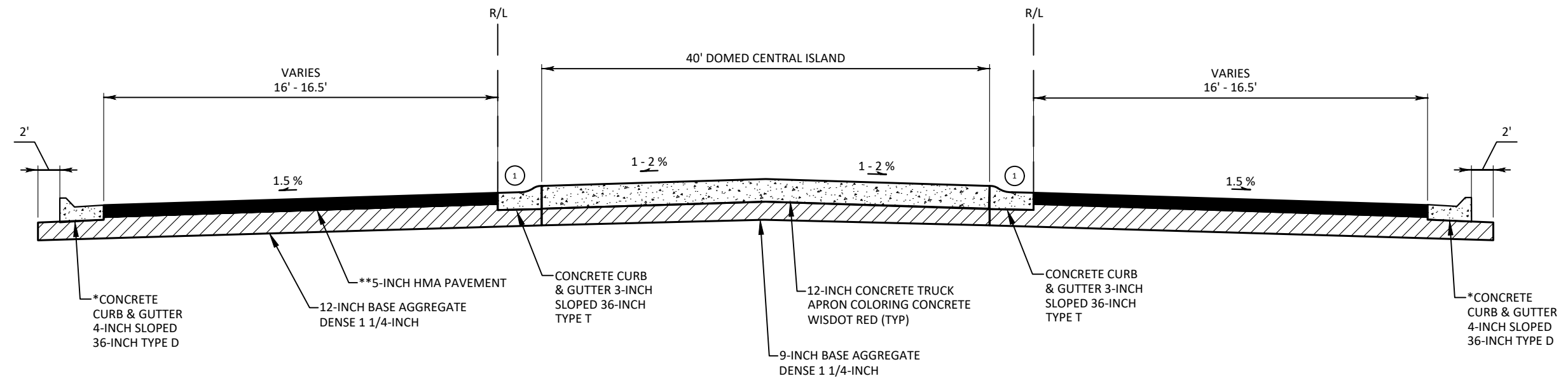
1 REVERSE SLOPE GUTTER SAME AS ADJACENT PAVEMENT

*SEE PLAN DETAILS FOR CURB & GUTTER TYPE LOCATIONS.
CONCRETE CURB & GUTTER 30-INCH TYPE D
CONCRETE CURB & GUTTER 4-INCH SLOPED 36-INCH TYPE D

**5" HMA PAVEMENT TO BE CONSTRUCTED IN 2 LAYER
-2.5" HMA PAVEMENT 4 LT 58-28 S UPPER LAYER
-2.5" HMA PAVEMENT 3 LT 58-28 S LOWER LAYER

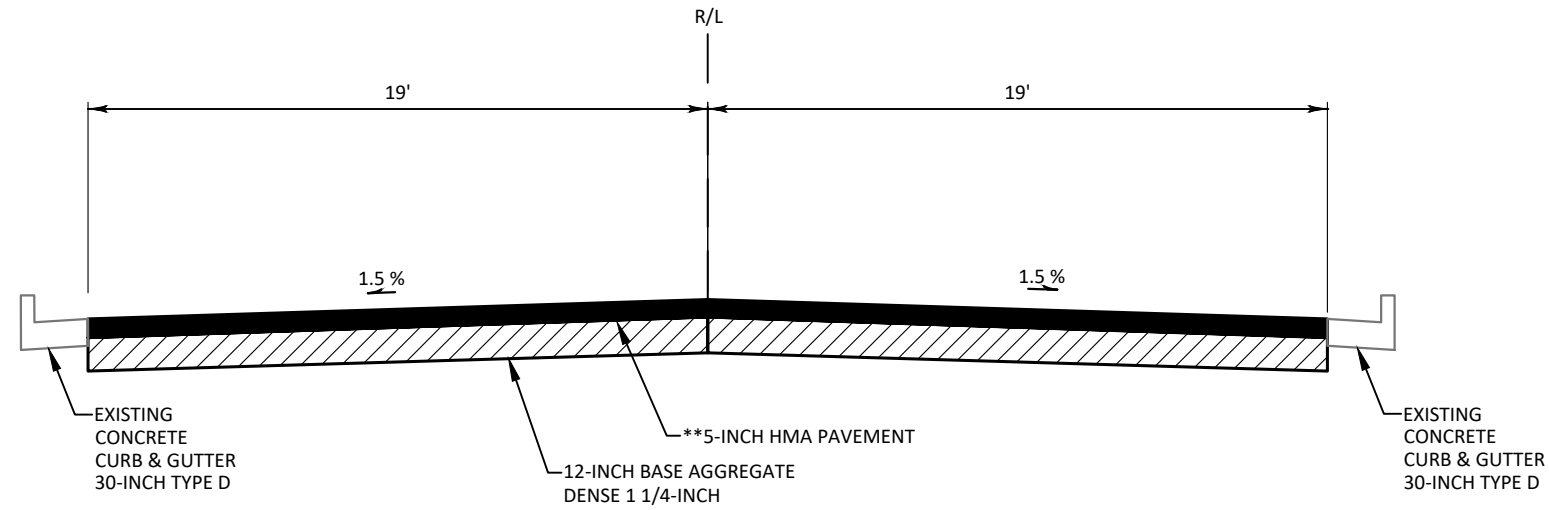


PROPOSED TYPICAL SECTION - SPLITTER ISLAND SECTION
 STA 34+90.20 RCA - STA 35+92 RCA
 STA 36+67.64 RCC - STA 37+44.27 RCC
 STA 500+18.97 EA - STA 500+91.12 EA
 STA 501+67.09 EA - STA 502+58.16 EA

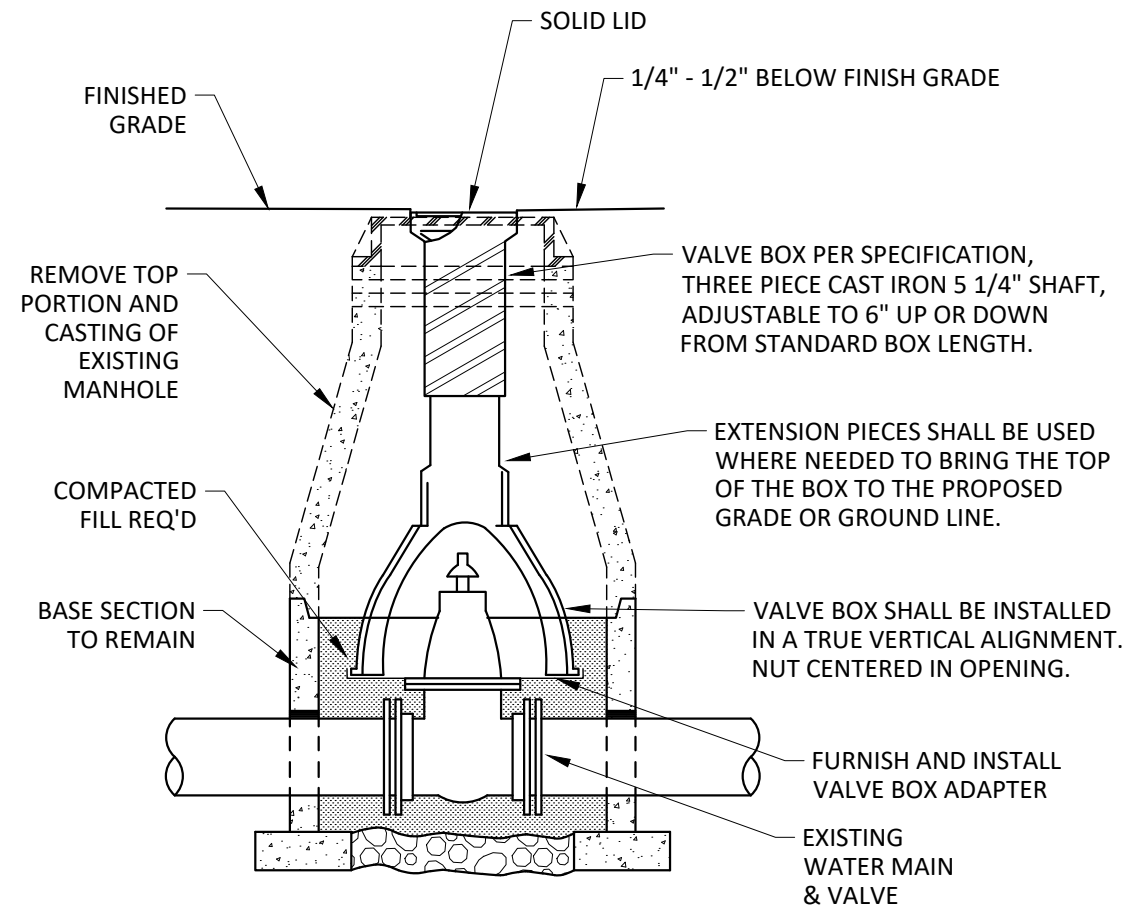


PROPOSED TYPICAL SECTION - DOMED CENTRAL ISLAND
 STA 10+00 CID - STA 11+44.51 CID

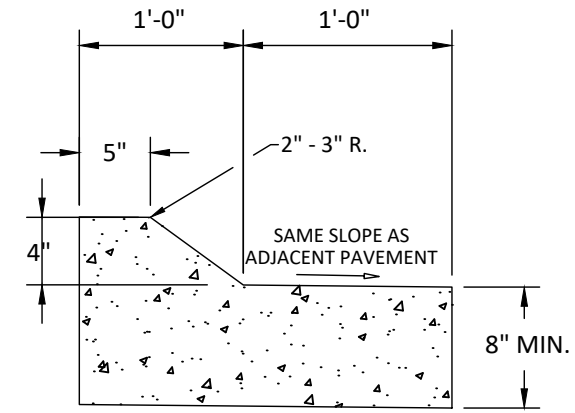
**5" HMA PAVEMENT TO BE CONSTRUCTED IN 2 LAYER
-2.5" HMA PAVEMENT 4 LT 58-28 S UPPER LAYER
-2.5" HMA PAVEMENT 3 LT 58-28 S LOWER LAYER



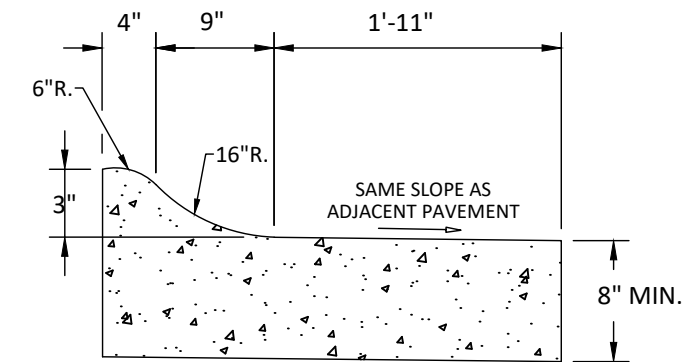
PROPOSED TYPICAL SECTION - EAST AVENUE N & RIDERS CLUB ROAD
 STA 34+84.98 RCA - STA 34+90.20 RCA
 STA 37+44.27 RCC - STA 37+50 RCC
 STA 500+13.75 EA - STA 500+18.97 EA
 STA 502+58.16 EA - STA 502+90.16 EA



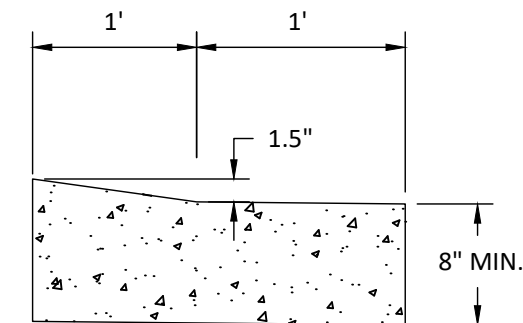
CONVERT MANHOLE TO VALVE BOX DETAIL



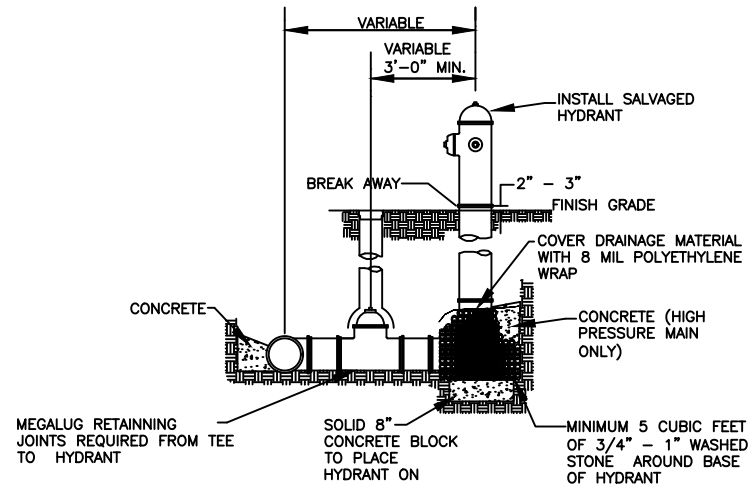
CONCRETE CURB & GUTTER 4-INCH SLOPED 24-INCH TYPE D



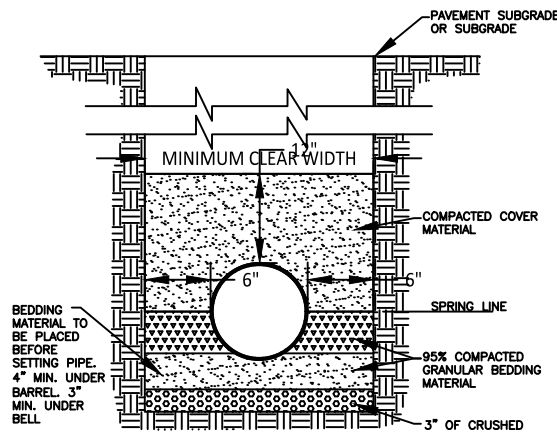
CONCRETE CURB & GUTTER 3-INCH SLOPED 36-INCH TYPE T



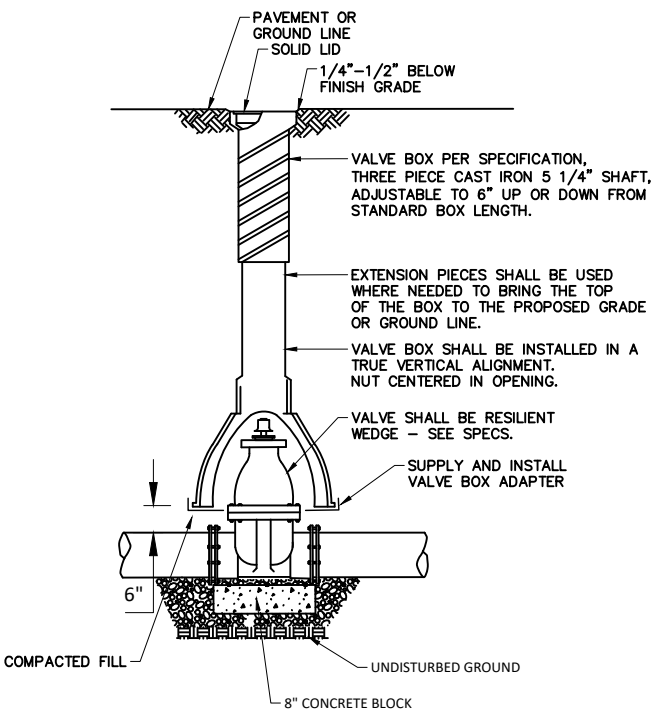
CONCRETE CURB & GUTTER DRIVEWAY CURB - IN SPLITTER ISLAND



HYDRANT & RUN DETAIL



CLASS "B" BEDDING DETAIL



GATE VALVE AND BOX

ALL WATER MAIN SHALL BE INSTALLED WITH APPROVED JOINT RESTRAINT CLAMPS, RODS, AND DEVICES AS ACCEPTED BY THE ENGINEER AT EACH FITTING. BASED UPON A 150 PSI TEST PRESSURE AND 8.0' COVER. THE FOLLOWING SHOWS THE MINIMUM LENGTH OF PIPE EACH WAY OF A FITTING TO BE RESTRAINED.

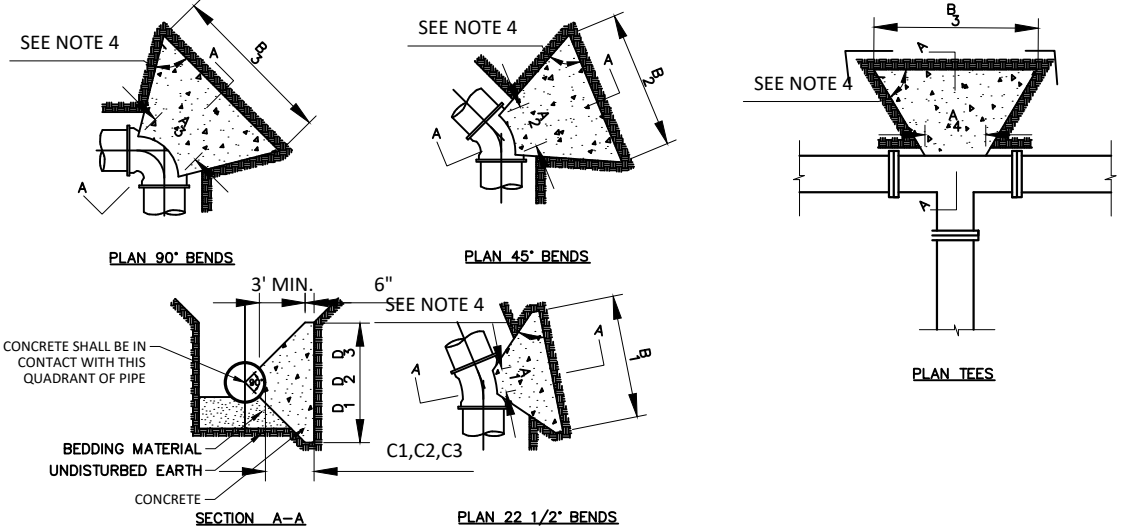
| PIPE DIAMETER | DEAD END TEE BRANCH OR 90° BEND | 45° BEND | 22 1/2° BEND |
|---------------|---------------------------------|----------|--------------|
| 6" | 23 LF | 7 LF | 0 LF |
| 8" | 30 LF | 10 LF | 2 LF |
| 10" | 38 LF | 12 LF | 3 LF |
| 12" | 50 LF | 14 LF | 3 LF |
| 16" | 72 LF | 24 LF | 10 LF |

WHERE RESTRAINED JOINTS ARE REQUIRED AND THE PIPE IS IN A CASING NEAR THE FITTING TO BE RESTRAINED, THE LENGTH OF PIPE IN THE CASING SHALL NOT BE INCLUDED IN THE LENGTH OF PIPE NECESSARY TO DEVELOP SUFFICIENT SOIL FRICTION TO OVERCOME THRUST.

ALL HYDRANTS, VALVES, SERVICES, FITTINGS AND PLUGS, STUBS AND EXTENSIONS, AND BLIND FLANGES USED FOR RETAINING WATER PRESSURE MUST BE TIED WITH THE RODS OR RISER CLAMPS AND MEGA LUG COMBINATION AS A PRESSURE RESTRAINT. ALL TIE RODS OR BARE METALS SHALL HAVE TWO COATS OF BITUMASTIC OR ACCEPTED EQUAL.

THRUST RESTRAINTS

| PIPE SIZE | BUTTRESS DIMENSIONS | | | | | |
|-----------|---------------------|--------|-------------|--------|-----------------|-------|
| | 22 1/2° BEND B1 | D1 | 45° BEND B2 | D2 | 90° BEND/TEE B3 | D3 |
| 6" | 1'-5" | 1'-5" | 1'-5" | 1'-5" | 2'-1" | 1'-6" |
| 8" | 1'-5" | 1'-5" | 2'-1" | 1'-6" | 2'-8" | 2'-0" |
| 12" | 1'-10" | 1'-10" | 3'-4" | 2'-0" | 4'-9" | 2'-6" |
| 16" | 3'-0" | 2'-0" | 3'-10" | 3'-0" | 6'-2" | 3'-6" |
| 20" | 3'-6" | 2'-8" | 5'-6" | 3'-4" | 8'-4" | 4'-0" |
| 24" | 4'-4" | 3'-0" | 6'-10" | 3'-10" | 9'-8" | 5'-0" |
| 30" | - | - | 9'-3" | 6'-0" | 17'-0" | 6'-0" |

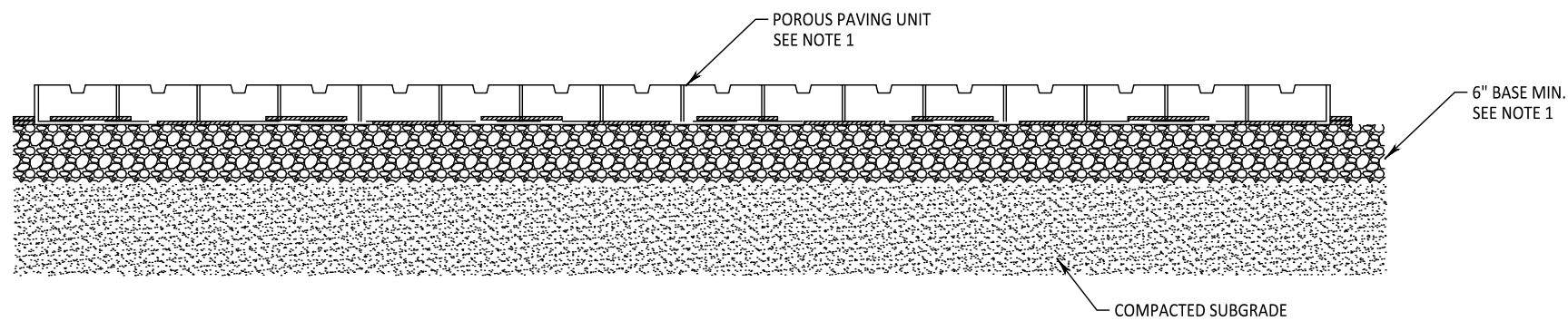


CONCRETE THRUST BLOCK

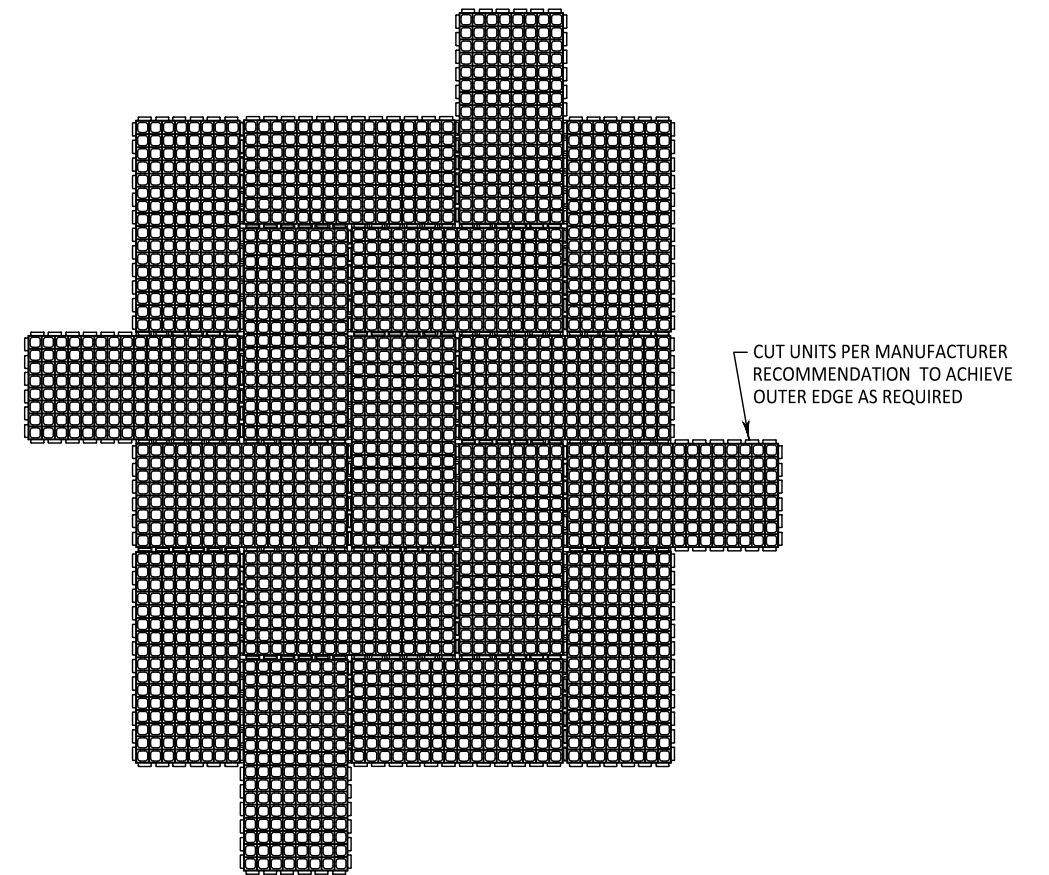
- NOTES:
- SHAPE OF BACK BUTTRESS MAY VARY AS LONG AS POURED AGAINST FIRM UNDISTURBED EARTH.
 - DIMENSION C1,C2,C3 SHOULD BE LARGE ENOUGH TO MAKE ANGLE D EQUAL TO OR LARGER THAN 45°.
 - DIMENSION A1,A2,A3, & A4 SHOULD BE AS LARGE AS POSSIBLE WITHOUT INTERFERING WITH MJ BOLTS.
 - 45° MINIMUM.
 - PLACE POLYETHYLENE BETWEEN CONCRETE AND PIPE.

NOTES:

- 1. THE BASE MATERIAL SHALL BE A HOMOGENOUS MIXTURE CONSISTING OF BASE AGGREGATE OPEN-GRADED BLENDED WITH PULVERIZED TOPSOIL AND VOID COMPONENT GENERALLY CONTAINING AIR AND/OR WATER. THIS HOMOGENOUS MIXTURE SHALL PROMOTE VEGETATIVE GROWTH AND PROVIDE REQUIRED STRUCTURAL SUPPORT. THE PERCENTAGE VOID SPACE OF THE AGGREGATE PORTION WHEN COMPACTED SHALL BE AT 30% MINIMUM. THE PULVERIZED TOPSOIL PORTION SHALL EQUAL APPROXIMATELY ONE-THIRD OF THE TOTAL VOLUME AND BE ADDED AND BLENDED TO PRODUCE A HOMOGENOUS MIXTURE PRIOR TO PLACEMENT OR WASHED INTO THE IN-PLACE COMPACTED AGGREGATE. ONCE PLACED, THE MIXTURE SHALL BE COMPACTED.



SECTION

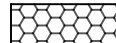



PREFERRED LAYOUT - HERRINGBONE PATTERN

VEGETATED POROUS PAVEMENT

LEGEND:PLAN DETAIL

- (AP27) 5.0-INCH HMA PAVEMENT
- (CG01) CONCRETE CURB & GUTTER 4-INCH SLOPED 24-INCH TYPE D
- (CG02) CONCRETE CURB & GUTTER 3-INCH SLOPED 36-INCH TYPE T
- (CG03) CONCRETE CURB AND GUTTER 4-INCH SLOPED 36-INCH TYPE D
- (CG07) CONCRETE CURB & GUTTER 30-INCH TYPE D
- (CS04) CONCRETE SIDEWALK 4-INCH
- (CS06) CONCRETE SIDEWALK 6-INCH
- (CT02) CONCRETE TRUCK APRON 12-INCH (RED)
- (DR04) CONCRETE DRIVEWAY, 6-INCH

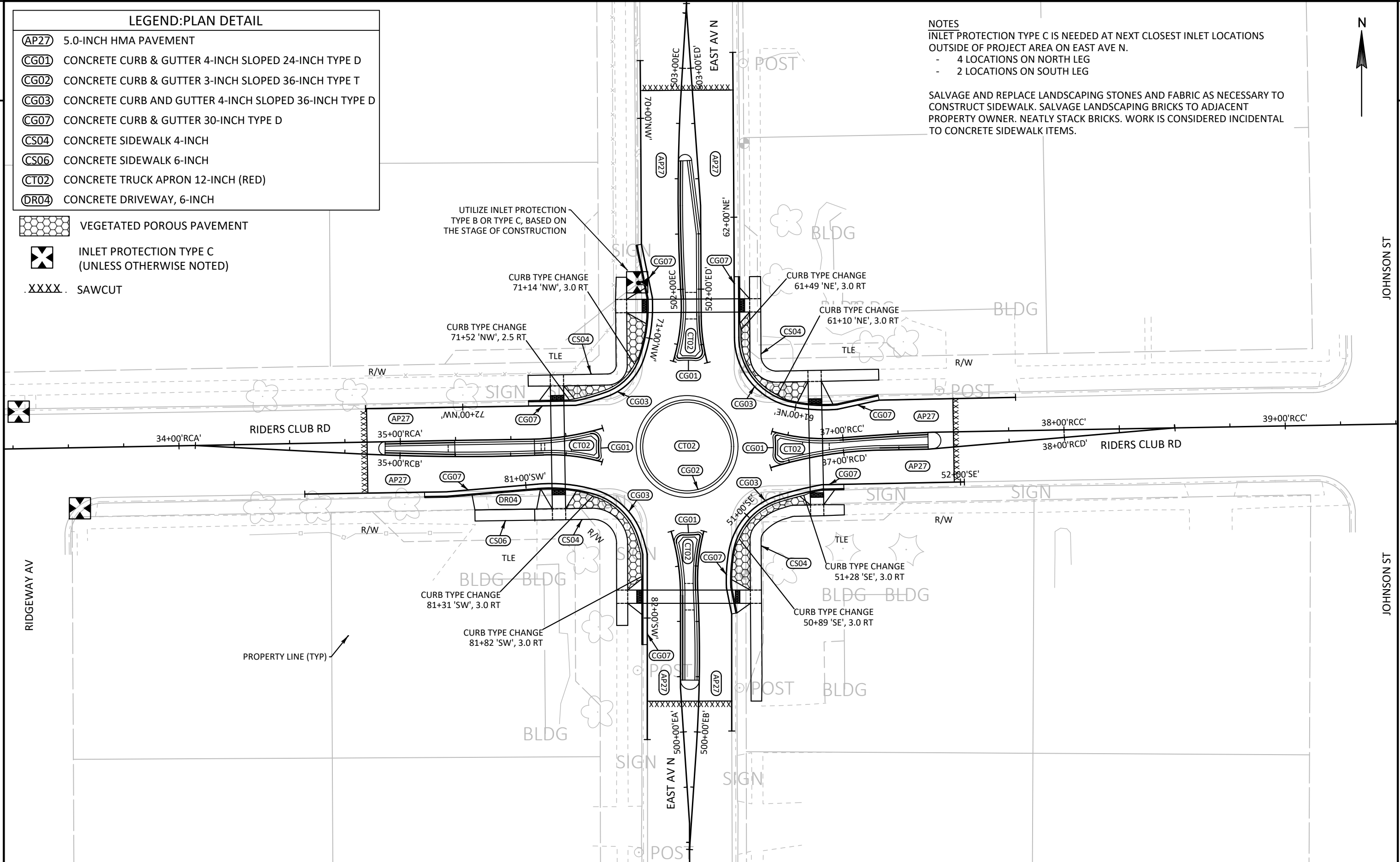
-  VEGETATED POROUS PAVEMENT
-  INLET PROTECTION TYPE C (UNLESS OTHERWISE NOTED)
- .XXXX. SAWCUT

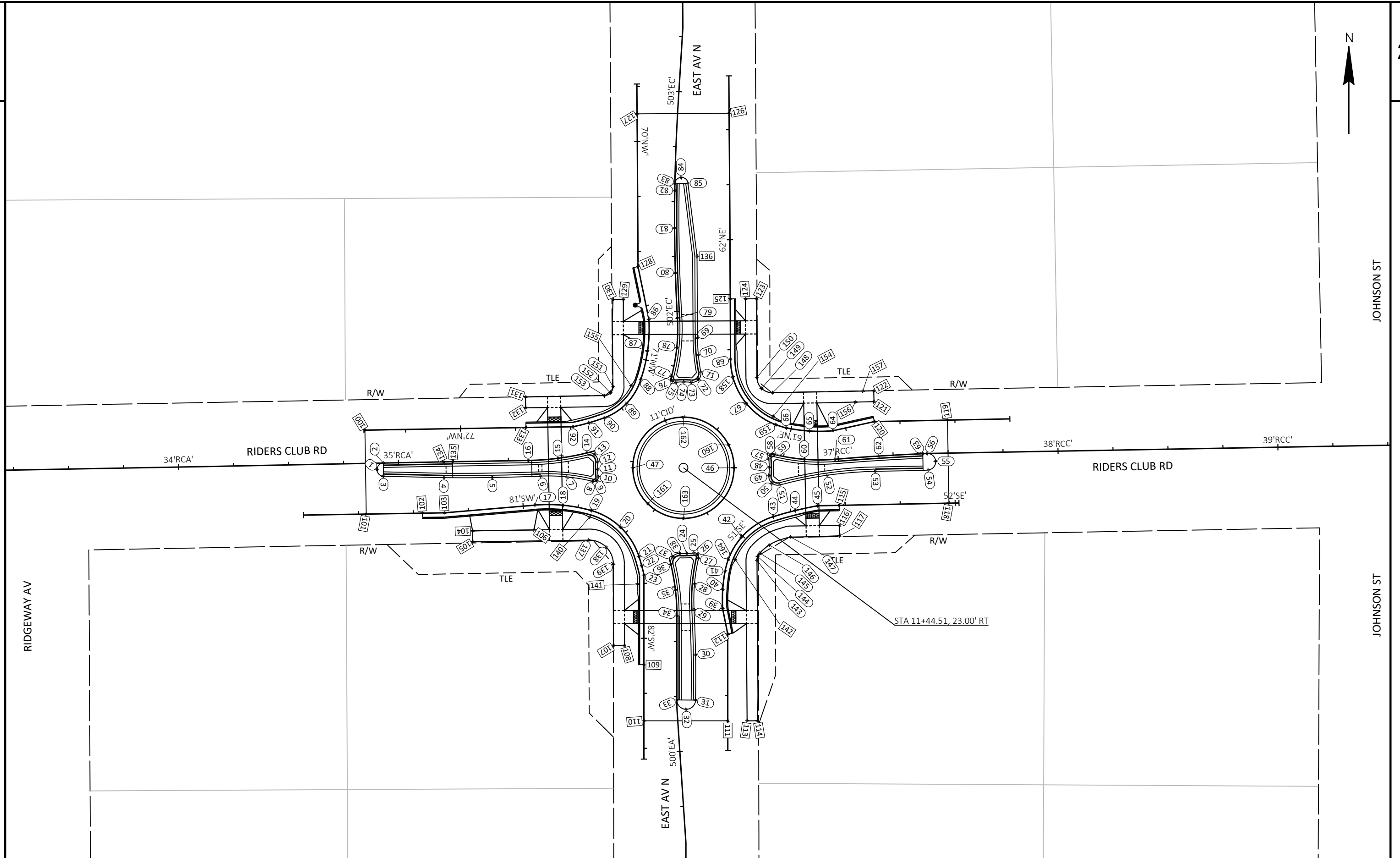
NOTES

INLET PROTECTION TYPE C IS NEEDED AT NEXT CLOSEST INLET LOCATIONS OUTSIDE OF PROJECT AREA ON EAST AVE N.

- 4 LOCATIONS ON NORTH LEG
- 2 LOCATIONS ON SOUTH LEG

SALVAGE AND REPLACE LANDSCAPING STONES AND FABRIC AS NECESSARY TO CONSTRUCT SIDEWALK. SALVAGE LANDSCAPING BRICKS TO ADJACENT PROPERTY OWNER. NEATLY STACK BRICKS. WORK IS CONSIDERED INCIDENTAL TO CONCRETE SIDEWALK ITEMS.





| | | | | | |
|------------------------|--------------------|-------------------|--------------|-------|---|
| PROJECT NO: 5991-02-62 | HWY: EAST AVENUE N | COUNTY: LA CROSSE | PLAN DETAILS | SHEET | E |
|------------------------|--------------------|-------------------|--------------|-------|---|

| STATION & OFFSET TABLE | | | | |
|------------------------|-----------|-----------|----------|-----------|
| POINT | STATION | ALIGNMENT | OFFSET | ELEVATION |
| 1 | 34+90.20 | 'RCA' | 2.91 RT | 717.86' |
| 2 | 34+93.20 | 'RCA' | 0.00 | 717.80' |
| 3 | 34+93.02 | 'RCA' | 5.99 RT | 717.70' |
| 4 | 35+20.59 | 'RCA' | 7.21 RT | 717.77' |
| 5 | 35+42.59 | 'RCA' | 7.48 RT | 717.82' |
| 6 | 35+64.25 | 'RCA' | 7.34 RT | 717.87' |
| 7 | 35+75.33 | 'RCA' | 8.95 RT | 717.90' |
| 8 | 35+85.54 | 'RCA' | 13.20 RT | 717.93' |
| 9 | 35+87.20 | 'RCA' | 13.21 RT | 717.94' |
| 10 | 35+88.18 | 'RCA' | 11.72 RT | 717.95' |
| 11 | 35+88.79 | 'RCA' | 8.46 RT | 717.98' |
| 12 | 35+89.70 | 'RCA' | 5.27 RT | 718.00' |
| 13 | 35+88.42 | 'RCA' | 2.25 RT | 0.00' |
| 14 | 35+86.04 | 'RCA' | 0.00 | 718.02' |
| 15 | 35+72.57 | 'RCA' | 0.00 | 717.99' |
| 16 | 35+59.10 | 'RCA' | 0.00 | 717.96' |
| 17 | 81+05.27 | 'SW' | 0.00 | 717.64' |
| 18 | 81+18.22 | 'SW' | 0.00 | 717.68' |
| 19 | 81+31.16 | 'SW' | 0.00 | 717.71' |
| 20 | 81+46.86 | 'SW' | 0.00 | 717.81' |
| 21 | 81+62.56 | 'SW' | 0.00 | 717.67' |
| 22 | 81+66.90 | 'SW' | 0.00 | 717.56' |
| 23 | 81+71.24 | 'SW' | 0.00 | 717.43' |
| 24 | 500+88.20 | 'EA' | 8.24 RT | 717.91' |
| 25 | 500+87.68 | 'EA' | 11.37 RT | 717.93' |
| 26 | 500+86.73 | 'EA' | 12.88 RT | 717.93' |
| 27 | 500+85.10 | 'EA' | 12.90 RT | 717.87' |
| 28 | 500+75.13 | 'EA' | 9.00 RT | 717.59' |
| 29 | 500+64.37 | 'EA' | 7.85 RT | 717.31' |
| 30 | 500+44.00 | 'EA' | 8.51 RT | 716.78' |
| 31 | 500+23.42 | 'EA' | 8.50 RT | 716.12' |
| 32 | 500+18.97 | 'EA' | 4.14 RT | 716.07' |
| 33 | 500+23.49 | 'EA' | 0.00 | 716.13' |
| 34 | 500+61.75 | 'EA' | 0.00 | 717.29' |
| 35 | 500+73.55 | 'EA' | 0.00 | 717.52' |
| 36 | 500+85.35 | 'EA' | 0.00 | 717.75' |
| 37 | 500+88.49 | 'EA' | 1.64 RT | 717.87' |
| 38 | 500+89.00 | 'EA' | 5.17 RT | 717.89' |
| 39 | 50+64.87 | 'SE' | 0.00 | 717.06' |
| 40 | 50+73.44 | 'SE' | 0.00 | 717.34' |

| STATION & OFFSET TABLE | | | | |
|------------------------|-----------|-----------|----------|-----------|
| POINT | STATION | ALIGNMENT | OFFSET | ELEVATION |
| 41 | 50+82.01 | 'SE' | 0.00 | 717.65' |
| 42 | 50+99.32 | 'SE' | 0.00 | 718.16' |
| 43 | 51+16.64 | 'SE' | 0.00 | 718.42' |
| 44 | 51+27.01 | 'SE' | 0.00 | 718.58' |
| 45 | 51+37.38 | 'SE' | 0.00 | 718.75' |
| 46 | 10+00.00 | 'CI' | 0.00 | 718.75' |
| 47 | 10+72.26 | 'CI' | 0.00 | 718.30' |
| 48 | 36+69.89 | 'RCC' | 6.27 RT | 718.83' |
| 49 | 36+70.59 | 'RCC' | 9.95 RT | 718.78' |
| 50 | 36+72.50 | 'RCC' | 12.96 RT | 718.79' |
| 51 | 36+75.71 | 'RCC' | 13.02 RT | 718.74' |
| 52 | 36+94.63 | 'RCC' | 7.02 RT | 719.06' |
| 53 | 37+16.36 | 'RCC' | 6.83 RT | 719.66' |
| 54 | 37+40.78 | 'RCC' | 7.49 RT | 720.65' |
| 55 | 37+44.27 | 'RCC' | 3.57 RT | 720.90' |
| 56 | 37+40.43 | 'RCC' | 0.00 LT | 720.72' |
| 57 | 36+68.80 | 'RCC' | 2.70 RT | 718.82' |
| 58 | 36+69.00 | 'RCC' | 0.87 RT | 718.83' |
| 59 | 36+70.62 | 'RCC' | 0.00 | 718.84' |
| 60 | 36+84.55 | 'RCC' | 0.00 | 719.02' |
| 61 | 36+98.48 | 'RCC' | 0.00 | 719.29' |
| 62 | 37+18.49 | 'RCC' | 0.00 | 719.87' |
| 63 | 37+38.49 | 'RCC' | 0.00 | 720.64' |
| 64 | 60+80.93 | 'NE' | 0.00 | 719.19' |
| 65 | 60+91.62 | 'NE' | 0.00 | 718.91' |
| 66 | 61+02.30 | 'NE' | 0.00 | 718.79' |
| 67 | 61+23.97 | 'NE' | 0.00 | 718.68' |
| 68 | 61+45.64 | 'NE' | 0.00 | 718.45' |
| 69 | 501+88.34 | 'EC' | 9.07 RT | 718.47' |
| 70 | 501+81.44 | 'EC' | 10.12 RT | 718.49' |
| 71 | 501+74.91 | 'EC' | 12.46 RT | 718.51' |
| 72 | 501+71.80 | 'EC' | 12.27 RT | 718.55' |
| 73 | 501+69.96 | 'EC' | 9.36 RT | 718.47' |
| 74 | 501+69.27 | 'EC' | 5.98 RT | 718.43' |
| 75 | 501+68.25 | 'EC' | 2.70 RT | 718.40' |
| 76 | 501+68.45 | 'EC' | 0.87 RT | 718.38' |
| 77 | 501+70.23 | 'EC' | 0.00 | 718.36' |
| 78 | 501+83.54 | 'EC' | 0.00 | 718.33' |
| 79 | 501+97.01 | 'EC' | 0.00 | 718.30' |
| 80 | 502+17.44 | 'EC' | 0.00 | 718.26' |

| STATION & OFFSET TABLE | | | | |
|------------------------|-----------|-----------|----------|-----------|
| POINT | STATION | ALIGNMENT | OFFSET | ELEVATION |
| 81 | 502+37.86 | 'EC' | 0.00 | 718.21' |
| 82 | 502+54.94 | 'EC' | 0.00 LT | 718.17' |
| 83 | 502+72.02 | 'EC' | 0.00 RT | 718.17' |
| 84 | 502+75.63 | 'EC' | 3.40 RT | 718.23' |
| 85 | 502+72.41 | 'EC' | 7.18 RT | 718.26' |
| 86 | 70+81.36 | 'NW' | 0.00 | 718.09' |
| 87 | 70+95.82 | 'NW' | 0.00 | 718.15' |
| 88 | 71+09.03 | 'NW' | 0.00 | 718.20' |
| 89 | 71+22.18 | 'NW' | 0.00 | 718.22' |
| 90 | 71+34.08 | 'NW' | 0.00 | 718.07' |
| 91 | 71+41.44 | 'NW' | 0.00 | 717.96' |
| 92 | 71+48.81 | 'NW' | 0.00 | 717.88' |
| 100 | 72+43.50 | 'NW' | 0.00 | 717.31' |
| 101 | 80+28.36 | 'SW' | 0.00 | 717.36' |
| 102 | 80+54.12 | 'SW' | 0.00 | 717.49' |
| 103 | 80+64.12 | 'SW' | 0.00 | 717.54' |
| 104 | 80+76.12 | 'SW' | 9.25 RT | 718.24' |
| 105 | 80+75.77 | 'SW' | 14.23 RT | 718.32' |
| 106 | 81+03.97 | 'SW' | 11.19 RT | 718.40' |
| 107 | 82+03.38 | 'SW' | 13.87 RT | 717.32' |
| 108 | 82+03.34 | 'SW' | 8.87 RT | 717.25' |
| 109 | 82+12.00 | 'SW' | 0.00 | 716.32' |
| 110 | 82+37.52 | 'SW' | 0.10 LT | 715.67' |
| 111 | 50+13.56 | 'SE' | 0.03 LT | 715.23' |
| 112 | 50+52.93 | 'SE' | 0.00 | 716.81' |
| 113 | 50+20.00 | 'SE' | 8.72 RT | 715.90' |
| 114 | 50+20.00 | 'SE' | 13.50 RT | 715.98' |
| 115 | 51+47.23 | 'SE' | 0.00 | 719.02' |
| 116 | 51+47.23 | 'SE' | 9.21 RT | 719.70' |
| 117 | 51+47.23 | 'SE' | 13.82 RT | 719.77' |
| 118 | 51+97.18 | 'SE' | 0.04 LT | 720.67' |
| 119 | 60+28.36 | 'NE' | 0.00 | 720.73' |
| 120 | 60+61.62 | 'NE' | 0.00 | 719.79' |
| 121 | 60+61.62 | 'NE' | 9.08 RT | 720.45' |
| 122 | 60+61.62 | 'NE' | 13.96 RT | 720.66' |
| 123 | 61+73.12 | 'NE' | 11.92 RT | 718.77' |
| 124 | 61+73.12 | 'NE' | 6.92 RT | 718.70' |
| 125 | 61+73.12 | 'NE' | 0.00 | 718.10' |
| 126 | 62+57.49 | 'NE' | 0.01 LT | 717.75' |
| 127 | 69+87.43 | 'NW' | 0.00 | 717.75' |

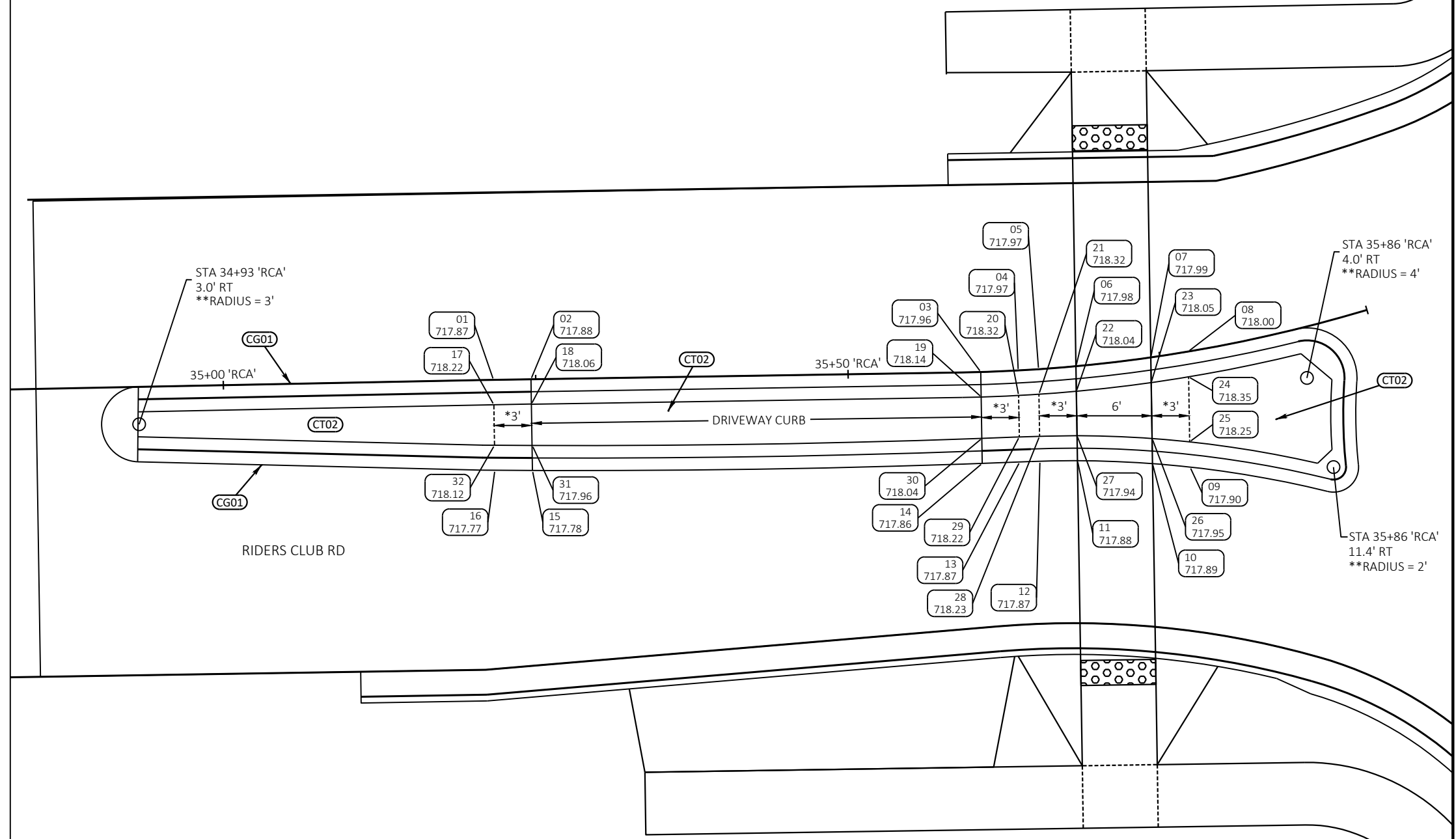
| STATION & OFFSET TABLE | | | | |
|------------------------|-----------|-----------|----------|-----------|
| POINT | STATION | ALIGNMENT | OFFSET | ELEVATION |
| 128 | 70+56.89 | 'NW' | 0.00 | 718.00' |
| 129 | 70+70.13 | 'NW' | 9.67 RT | 718.75' |
| 130 | 70+69.15 | 'NW' | 14.57 RT | 718.81' |
| 131 | 71+70.26 | 'NW' | 14.18 RT | 718.41' |
| 132 | 71+70.26 | 'NW' | 9.18 RT | 718.34' |
| 133 | 71+70.26 | 'NW' | 0.00 | 717.66' |
| 134 | 35+20.62 | 'RCA' | 0.00 | 717.87' |
| 135 | 35+24.62 | 'RCA' | 0.00 | 717.88' |
| 136 | 502+37.85 | 'EC' | 10.16 RT | 718.33' |
| 137 | 81+35.26 | 'SW' | 13.13 RT | 718.08' |
| 138 | 81+48.62 | 'SW' | 11.14 RT | 718.03' |
| 139 | 81+61.94 | 'SW' | 12.35 RT | 718.01' |
| 140 | 81+31.39 | 'SW' | 3.00 RT | 718.15' |
| 141 | 81+81.51 | 'SW' | 3.00 RT | 717.56' |
| 142 | 50+88.54 | 'SE' | 3.00 RT | 718.28' |
| 143 | 50+94.05 | 'SE' | 11.99 RT | 718.34' |
| 144 | 50+96.62 | 'SE' | 11.26 RT | 718.38' |
| 145 | 50+99.40 | 'SE' | 11.00 RT | 718.43' |
| 146 | 51+08.02 | 'SE' | 11.00 RT | 718.60' |
| 147 | 51+21.76 | 'SE' | 11.17 RT | 718.86' |
| 148 | 61+17.91 | 'NE' | 11.96 RT | 719.06' |
| 149 | 61+25.31 | 'NE' | 9.66 RT | 718.98' |
| 150 | 61+33.12 | 'NE' | 10.30 RT | 718.93' |
| 151 | 71+19.74 | 'NW' | 9.92 RT | 718.45' |
| 152 | 71+24.04 | 'NW' | 8.75 RT | 718.41' |
| 153 | 71+28.53 | 'NW' | 9.44 RT | 718.39' |
| 154 | 61+09.89 | 'NE' | 3.00 RT | 719.01' |
| 155 | 71+13.67 | 'NW' | 3.00 RT | 718.58' |
| 158 | 61+37.67 | 'NE' | 0.00 | 718.55' |
| 159 | 61+07.19 | 'NE' | 0.00 | 718.77' |
| 160 | 11+33.71 | 'CI' | 0.00 | 718.78' |
| 161 | 10+54.02 | 'CI' | 0.00 | 718.20' |
| 162 | 11+08.38 | 'CI' | 0.00 | 718.64' |
| 163 | 10+36.13 | 'CI' | 0.00 | 718.30' |
| 164 | 50+87.41 | 'SE' | 0.00 | 717.84' |

| LEGEND: PLAN DETAIL | |
|---------------------|---|
| CG01 | CONCRETE CURB & GUTTER 4-INCH SLOPED 24-INCH TYPE D |
| CT02 | CONCRETE TRUCK APRON 12-INCH (RED) |

- * 10% MAX SLOPE IN TAPER
- ** RADIUS POINTS ARE TO FLAG OF CURB



| WEST ISLAND | | | | | |
|--------------|----------|----------|-----------|-----------|-----------|
| POINT NUMBER | STATION | OFFSET | ELEVATION | NORTHING | EASTING |
| 01 | 35+21.62 | 0.00' | 717.87 | 165624.00 | 449387.53 |
| 02 | 35+24.62 | 0.00' | 717.88 | 165624.05 | 449390.53 |
| 03 | 35+60.62 | 0.00' | 717.96 | 165624.63 | 449426.53 |
| 04 | 35+63.62 | 0.00' | 717.97 | 165624.75 | 449429.52 |
| 05 | 35+65.25 | 0.00' | 717.97 | 165624.85 | 449431.15 |
| 06 | 35+68.25 | 0.00' | 717.98 | 165625.09 | 449434.14 |
| 07 | 35+74.28 | 0.00' | 717.99 | 165625.80 | 449440.13 |
| 08 | 35+77.31 | 0.00' | 718.00 | 165626.26 | 449443.13 |
| 09 | 35+76.01 | 9.14' RT | 717.90 | 165617.02 | 449443.27 |
| 10 | 35+73.28 | 8.43' RT | 717.89 | 165617.30 | 449440.27 |
| 11 | 35+67.71 | 7.54' RT | 717.88 | 165617.53 | 449434.26 |
| 12 | 35+64.89 | 7.35' RT | 717.87 | 165617.49 | 449431.26 |
| 13 | 35+63.36 | 7.32' RT | 717.87 | 165617.43 | 449429.64 |
| 14 | 35+60.53 | 7.31' RT | 717.86 | 165617.32 | 449426.64 |
| 15 | 35+24.62 | 7.30' RT | 717.78 | 165616.75 | 449390.65 |
| 16 | 35+21.62 | 7.23' RT | 717.77 | 165616.77 | 449387.65 |
| 17 | 35+21.62 | 2.00' RT | 718.22 | 165622.00 | 449387.56 |
| 18 | 35+24.62 | 2.00' RT | 718.06 | 165622.05 | 449390.56 |
| 19 | 35+60.61 | 2.00' RT | 718.14 | 165622.63 | 449426.57 |
| 20 | 35+63.55 | 2.00' RT | 718.32 | 165622.75 | 449429.56 |
| 21 | 35+65.14 | 2.00' RT | 718.32 | 165622.85 | 449431.18 |
| 22 | 35+68.10 | 2.00' RT | 718.04 | 165623.08 | 449434.18 |
| 23 | 35+74.03 | 2.00' RT | 718.05 | 165623.78 | 449440.17 |
| 24 | 35+77.01 | 2.00' RT | 718.35 | 165624.24 | 449443.16 |
| 25 | 35+76.28 | 7.15' RT | 718.25 | 165619.04 | 449443.24 |
| 26 | 35+73.51 | 6.44' RT | 717.96 | 165619.31 | 449440.24 |
| 27 | 35+67.85 | 5.54' RT | 717.94 | 165619.53 | 449434.23 |
| 28 | 35+64.98 | 5.36' RT | 718.23 | 165619.49 | 449431.23 |
| 29 | 35+63.43 | 5.31' RT | 718.22 | 165619.43 | 449429.61 |
| 30 | 35+60.55 | 5.31' RT | 718.04 | 165619.32 | 449426.61 |
| 31 | 35+24.64 | 5.30' RT | 717.96 | 165618.75 | 449390.64 |
| 32 | 35+21.62 | 5.23' RT | 718.12 | 165618.77 | 449387.61 |

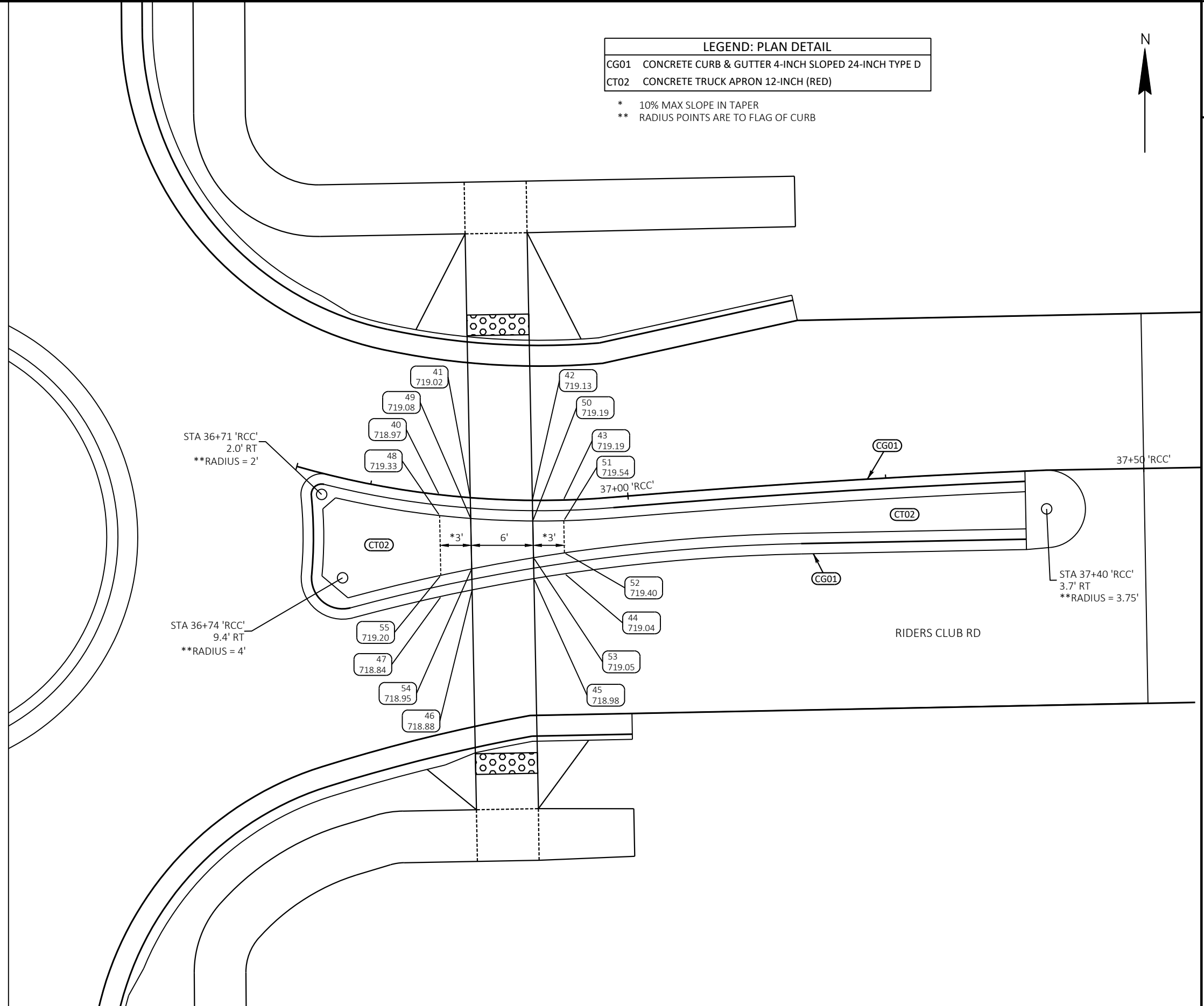


| LEGEND: PLAN DETAIL | |
|---------------------|---|
| CG01 | CONCRETE CURB & GUTTER 4-INCH SLOPED 24-INCH TYPE D |
| CT02 | CONCRETE TRUCK APRON 12-INCH (RED) |

- * 10% MAX SLOPE IN TAPER
- ** RADIUS POINTS ARE TO FLAG OF CURB



| EAST ISLAND | | | | | |
|--------------|----------|----------|-----------|-----------|-----------|
| POINT NUMBER | STATION | OFFSET | ELEVATION | NORTHING | EASTING |
| 40 | 36+81.70 | 0.00' | 718.97 | 165625.39 | 449547.70 |
| 41 | 36+84.72 | 0.00' | 719.02 | 165625.10 | 449550.71 |
| 42 | 36+90.74 | 0.00' | 719.13 | 165624.84 | 449556.71 |
| 43 | 36+93.74 | 0.00' | 719.19 | 165624.87 | 449559.71 |
| 44 | 36+93.68 | 7.13' RT | 719.04 | 165617.74 | 449559.85 |
| 45 | 36+90.93 | 7.57' RT | 718.98 | 165617.27 | 449556.86 |
| 46 | 36+85.51 | 8.88' RT | 718.88 | 165616.18 | 449550.88 |
| 47 | 36+82.88 | 9.76' RT | 718.84 | 165615.55 | 449547.89 |
| 48 | 36+81.97 | 2.00' RT | 719.33 | 165623.37 | 449547.74 |
| 49 | 36+84.92 | 2.00' RT | 719.08 | 165623.09 | 449550.75 |
| 50 | 36+90.79 | 2.00' RT | 719.19 | 165622.84 | 449556.75 |
| 51 | 36+93.72 | 2.00' RT | 719.54 | 165622.87 | 449559.75 |
| 52 | 36+93.70 | 5.12' RT | 719.40 | 165619.76 | 449559.81 |
| 53 | 36+90.88 | 5.55' RT | 719.05 | 165619.29 | 449556.82 |
| 54 | 36+85.35 | 6.86' RT | 718.95 | 165618.21 | 449550.84 |
| 55 | 36+82.66 | 7.74' RT | 719.20 | 165617.59 | 449547.85 |

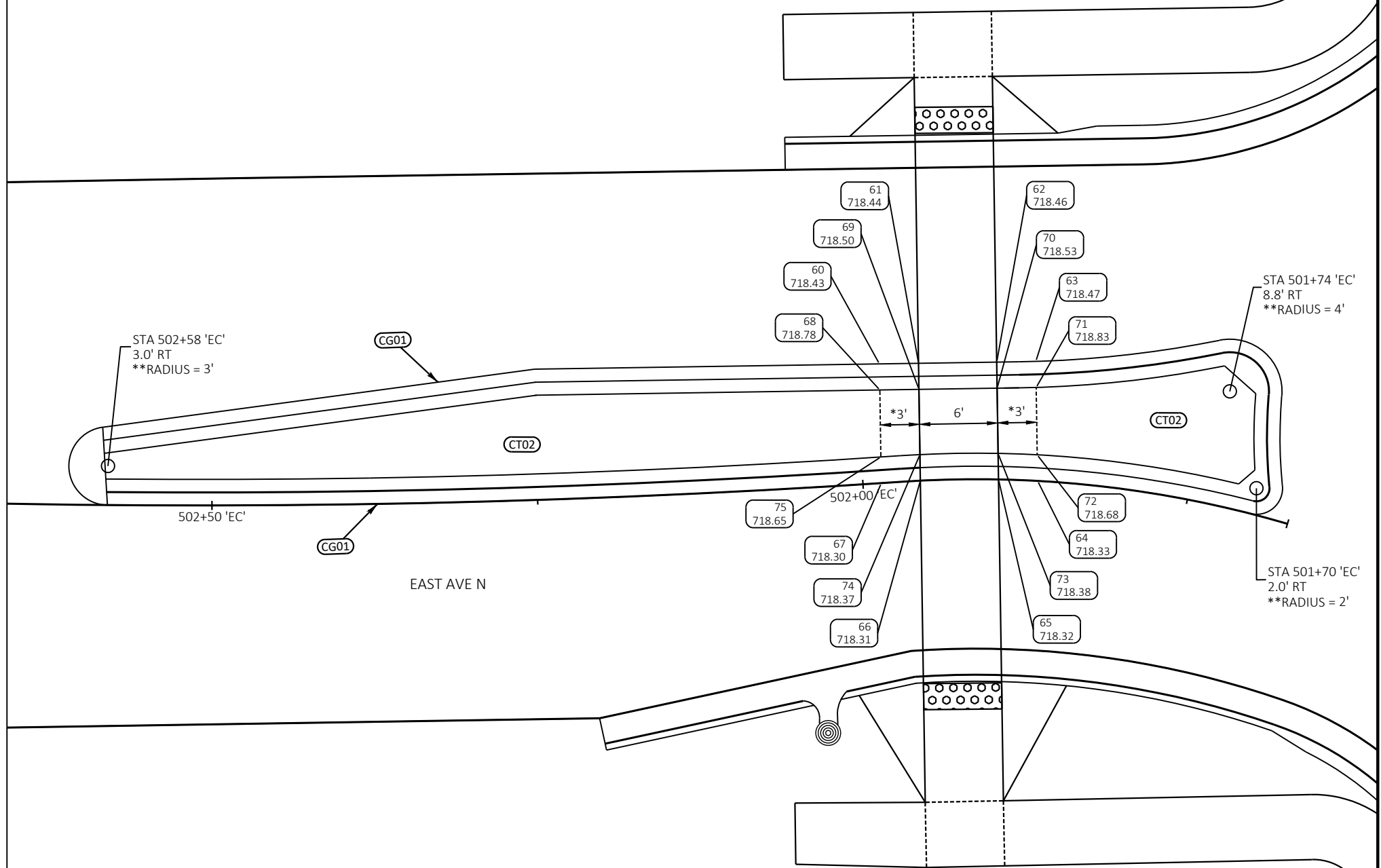


| LEGEND: PLAN DETAIL | |
|---------------------|---|
| CG01 | CONCRETE CURB & GUTTER 4-INCH SLOPED 24-INCH TYPE D |
| CT02 | CONCRETE TRUCK APRON 12-INCH (RED) |

- * 10% MAX SLOPE IN TAPER
- ** RADIUS POINTS ARE TO FLAG OF CURB



| NORTH ISLAND | | | | | |
|--------------|-----------|----------|-----------|-----------|-----------|
| POINT NUMBER | STATION | OFFSET | ELEVATION | NORTHING | EASTING |
| 60 | 501+98.13 | 9.14' RT | 718.43 | 165690.96 | 449501.85 |
| 61 | 501+95.30 | 9.00' RT | 718.44 | 165687.96 | 449501.87 |
| 62 | 501+89.88 | 9.00' RT | 718.46 | 165681.96 | 449501.90 |
| 63 | 501+87.17 | 9.15' RT | 718.47 | 165678.96 | 449501.93 |
| 64 | 501+86.59 | 0.00' | 718.33 | 165678.91 | 449492.75 |
| 65 | 501+89.59 | 0.00' | 718.32 | 165681.92 | 449492.90 |
| 66 | 501+95.59 | 0.00' | 718.31 | 165687.92 | 449492.86 |
| 67 | 501+98.60 | 0.02' LT | 718.30 | 165690.91 | 449492.69 |
| 68 | 501+98.23 | 7.14' RT | 718.78 | 165690.95 | 449499.85 |
| 69 | 501+95.36 | 7.00' RT | 718.50 | 165687.95 | 449499.87 |
| 70 | 501+89.82 | 7.00' RT | 718.53 | 165681.95 | 449499.90 |
| 71 | 501+87.05 | 7.16' RT | 718.83 | 165678.95 | 449499.93 |
| 72 | 501+86.72 | 2.00' RT | 718.68 | 165678.92 | 449494.76 |
| 73 | 501+89.66 | 2.00' RT | 718.38 | 165681.93 | 449494.90 |
| 74 | 501+95.52 | 2.00' RT | 718.37 | 165687.93 | 449494.87 |
| 75 | 501+98.49 | 2.00' RT | 718.65 | 165690.92 | 449494.71 |

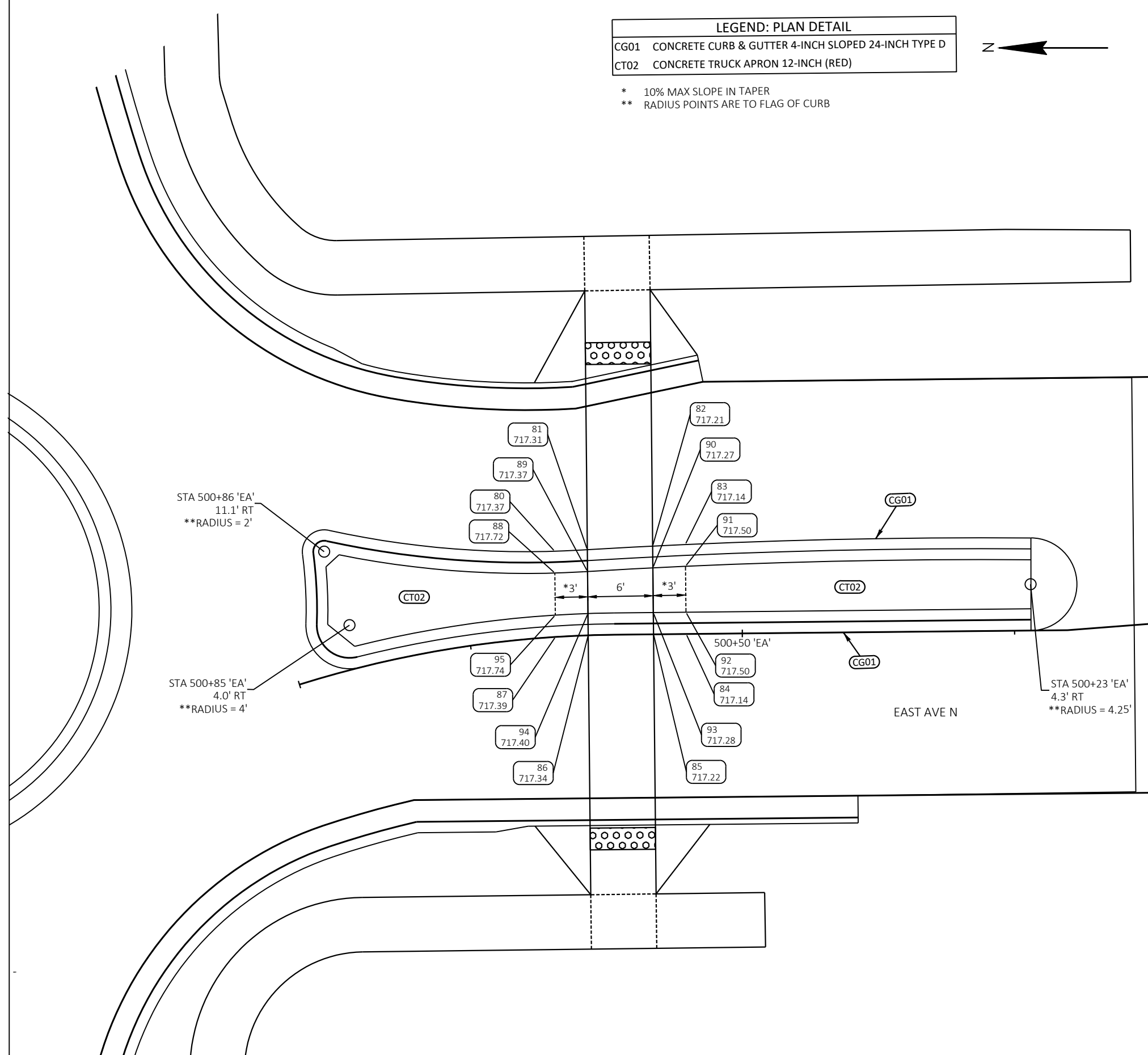


| LEGEND: PLAN DETAIL | |
|---------------------|---|
| CG01 | CONCRETE CURB & GUTTER 4-INCH SLOPED 24-INCH TYPE D |
| CT02 | CONCRETE TRUCK APRON 12-INCH (RED) |

- * 10% MAX SLOPE IN TAPER
- ** RADIUS POINTS ARE TO FLAG OF CURB

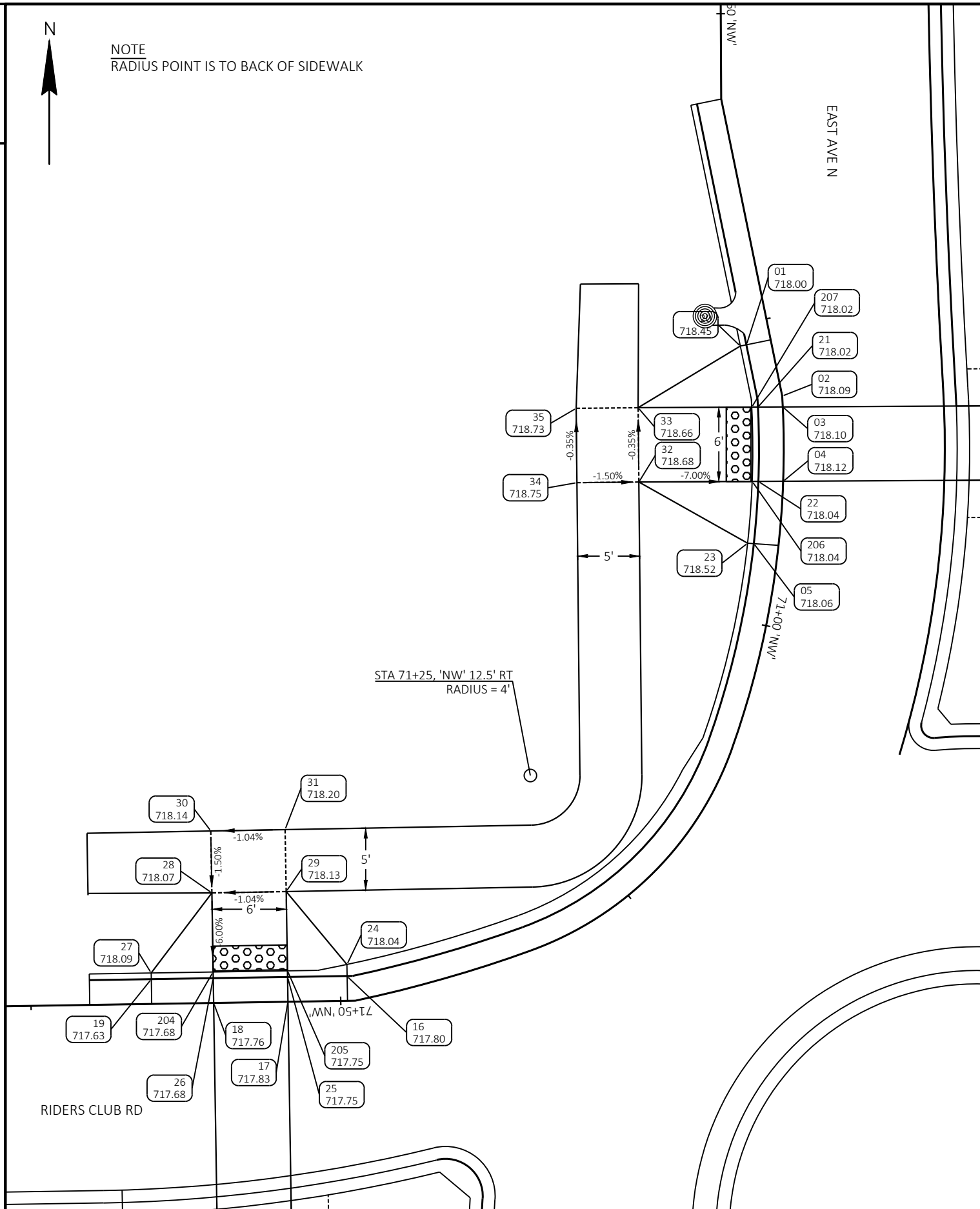


| SOUTH ISLAND | | | | | |
|--------------|-----------|----------|-----------|-----------|-----------|
| POINT NUMBER | STATION | OFFSET | ELEVATION | NORTHING | EASTING |
| 80 | 500+66.76 | 7.85' RT | 717.37 | 165559.41 | 449500.27 |
| 81 | 500+63.98 | 7.86' RT | 717.31 | 165556.41 | 449500.38 |
| 82 | 500+58.15 | 8.10' RT | 717.21 | 165550.41 | 449500.65 |
| 83 | 500+55.15 | 8.22' RT | 717.14 | 165547.41 | 449500.77 |
| 84 | 500+55.15 | 0.00' | 717.14 | 165547.41 | 449492.55 |
| 85 | 500+58.15 | 0.00' | 717.22 | 165550.41 | 449492.55 |
| 86 | 500+64.15 | 0.00' | 717.34 | 165556.41 | 449492.52 |
| 87 | 500+67.16 | 0.00' | 717.39 | 165559.41 | 449492.41 |
| 88 | 500+66.86 | 5.86' RT | 717.72 | 165559.41 | 449498.27 |
| 89 | 500+64.02 | 5.86' RT | 717.37 | 165556.41 | 449498.38 |
| 90 | 500+58.15 | 6.10' RT | 717.27 | 165550.41 | 449498.65 |
| 91 | 500+55.15 | 6.22' RT | 717.50 | 165547.41 | 449498.77 |
| 92 | 500+55.15 | 2.00' RT | 717.50 | 165547.41 | 449494.55 |
| 93 | 500+58.15 | 2.00' RT | 717.28 | 165550.41 | 449494.55 |
| 94 | 500+64.11 | 2.00' RT | 717.40 | 165556.41 | 449494.52 |
| 95 | 500+67.05 | 2.00' RT | 717.74 | 165559.41 | 449494.41 |



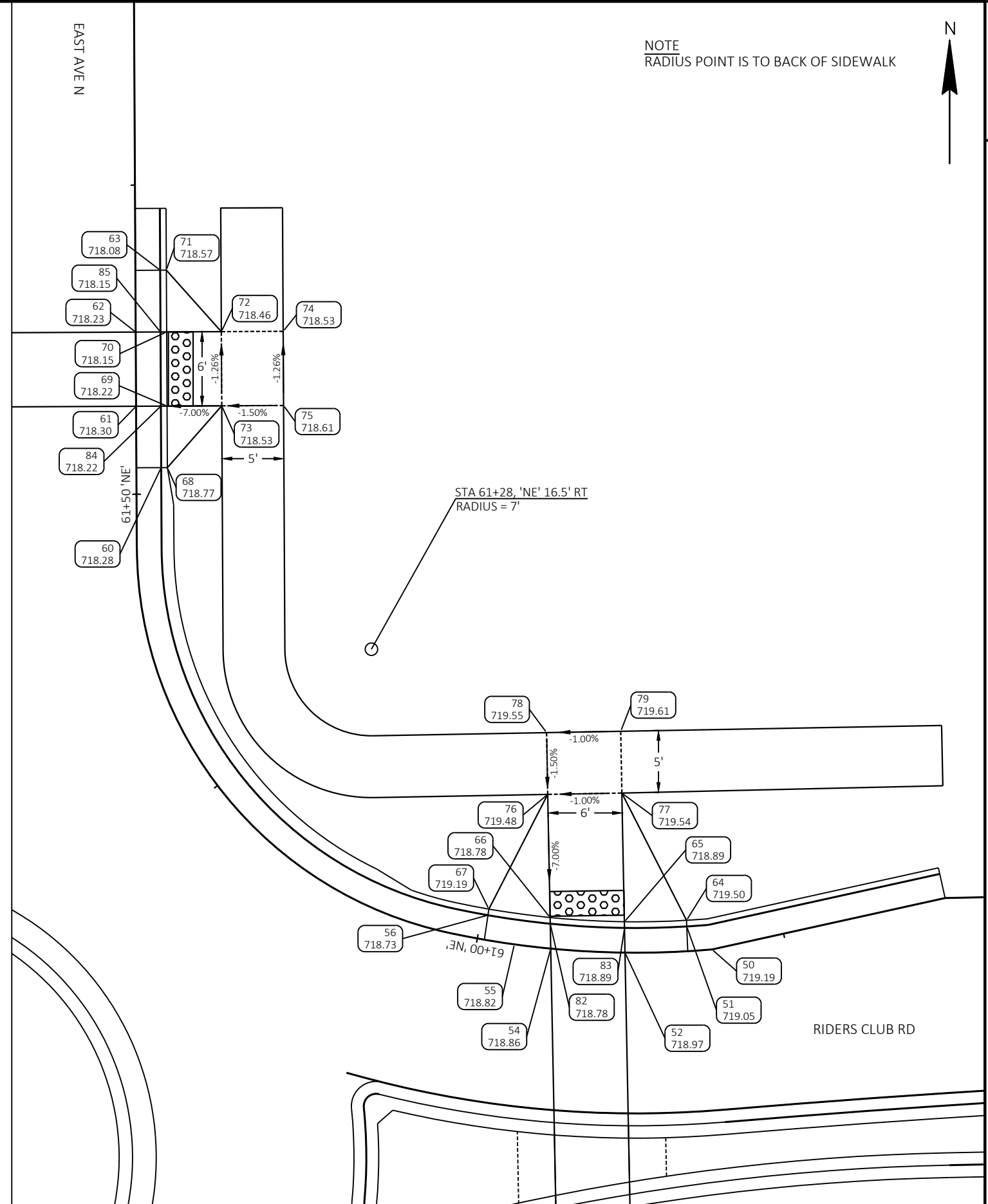


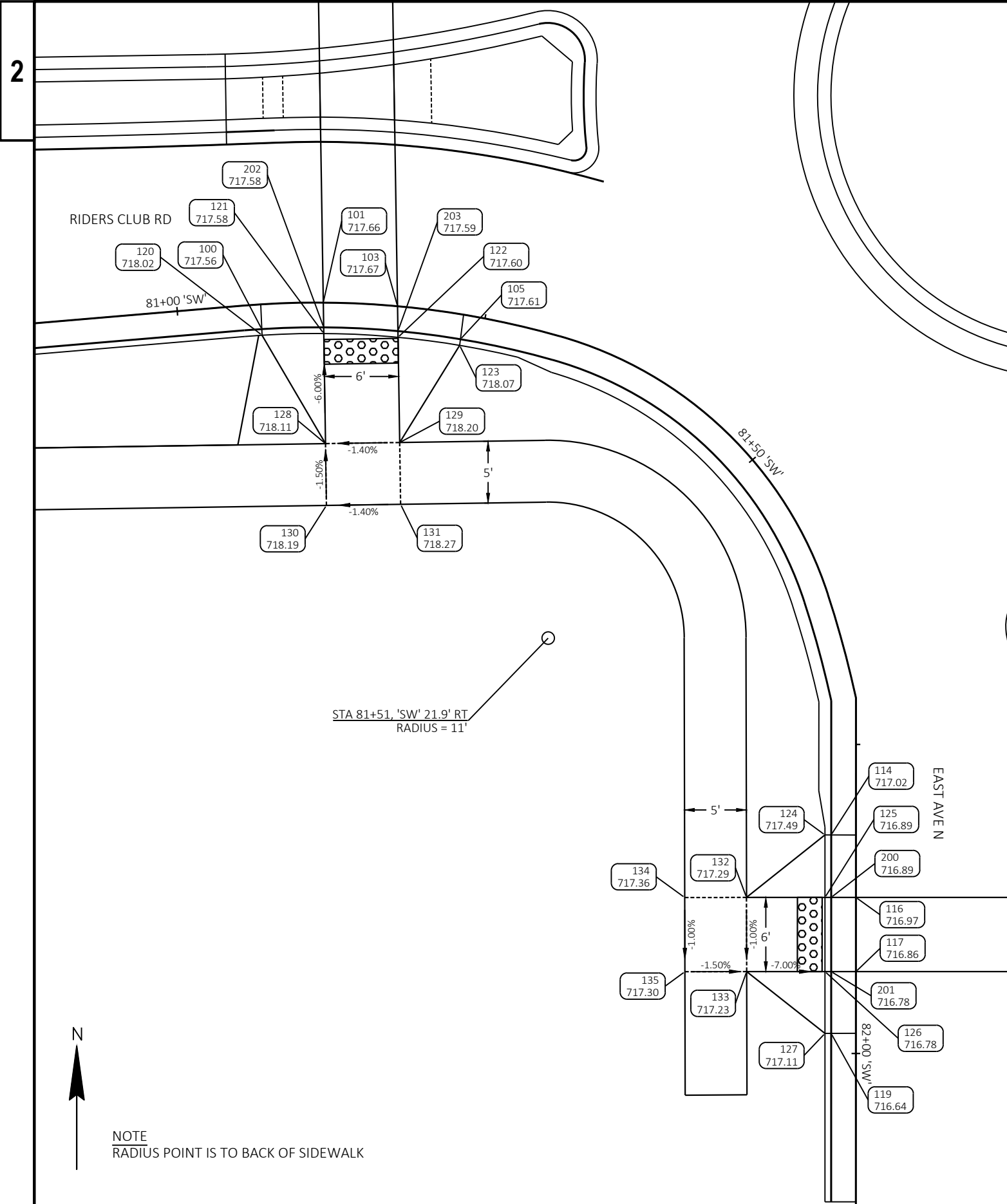
NOTE
RADIUS POINT IS TO BACK OF SIDEWALK



| CR-NW | | | | | |
|--------------|----------------|-----------|-----------|-----------|-----------|
| POINT NUMBER | STATION | OFFSET | ELEVATION | NORTHING | EASTING |
| 01 | 70+76.73 CR-NW | 2.00' RT | 718.00 | 165692.87 | 449476.92 |
| 02 | 70+81.36 CR-NW | 0.00' | 718.09 | 165688.73 | 449479.81 |
| 03 | 70+82.25 CR-NW | 0.00' RT | 718.10 | 165687.85 | 449479.85 |
| 04 | 70+88.25 CR-NW | 0.00' RT | 718.12 | 165681.85 | 449479.89 |
| 05 | 70+93.51 CR-NW | 2.00' RT | 718.06 | 165676.82 | 449477.51 |
| 16 | 71+49.45 CR-NW | 1.97' RT | 717.80 | 165641.89 | 449444.69 |
| 17 | 71+54.26 CR-NW | 0.00' | 717.83 | 165639.83 | 449439.91 |
| 18 | 71+60.26 CR-NW | 0.00' | 717.76 | 165639.74 | 449433.91 |
| 19 | 71+65.26 CR-NW | 2.00' RT | 717.63 | 165641.66 | 449428.88 |
| 20 | 70+76.73 CR-NW | 2.50' RT | 718.45 | 165692.77 | 449476.43 |
| 21 | 70+82.16 CR-NW | 2.00' RT | 718.02 | 165687.84 | 449477.85 |
| 22 | 70+88.34 CR-NW | 2.00' RT | 718.04 | 165681.84 | 449477.89 |
| 23 | 70+93.53 CR-NW | 2.50' RT | 718.52 | 165676.85 | 449477.02 |
| 24 | 71+49.45 CR-NW | 2.93' RT | 718.04 | 165642.84 | 449444.67 |
| 25 | 71+54.26 CR-NW | 2.00' RT | 717.75 | 165641.83 | 449439.88 |
| 26 | 71+60.26 CR-NW | 2.00' RT | 717.68 | 165641.74 | 449433.88 |
| 27 | 71+65.26 CR-NW | 2.50' RT | 718.09 | 165642.16 | 449428.87 |
| 28 | 71+60.26 CR-NW | 8.88' RT | 718.07 | 165648.62 | 449433.77 |
| 29 | 71+54.26 CR-NW | 8.90' RT | 718.13 | 165648.73 | 449439.77 |
| 30 | 71+60.26 CR-NW | 13.88' RT | 718.14 | 165653.62 | 449433.69 |
| 31 | 71+54.27 CR-NW | 13.90' RT | 718.20 | 165653.73 | 449439.68 |
| 32 | 70+88.83 CR-NW | 11.64' RT | 718.68 | 165681.79 | 449468.24 |
| 33 | 70+79.95 CR-NW | 11.58' RT | 718.66 | 165687.79 | 449468.18 |
| 34 | 70+89.18 CR-NW | 16.63' RT | 718.75 | 165681.74 | 449463.24 |
| 35 | 70+78.99 CR-NW | 16.49' RT | 718.73 | 165687.74 | 449463.18 |
| 204 | 71+60.26 CR-NW | 2.50' RT | 717.68 | 165642.24 | 449433.87 |
| 205 | 71+54.26 CR-NW | 2.50' RT | 717.75 | 165642.33 | 449439.87 |
| 206 | 70+88.36 CR-NW | 2.50' RT | 718.04 | 165681.84 | 449477.39 |
| 207 | 70+82.14 CR-NW | 2.50' RT | 718.02 | 165687.84 | 449477.35 |

| CR-NE | | | | | |
|--------------|----------------|-----------|-----------|-----------|-----------|
| POINT NUMBER | STATION | OFFSET | ELEVATION | NORTHING | EASTING |
| 50 | 60+80.93 CR-NE | 0.00' | 719.19 | 165638.10 | 449563.50 |
| 51 | 60+82.87 CR-NE | 2.01' RT | 719.05 | 165639.97 | 449561.45 |
| 52 | 60+87.98 CR-NE | 0.00' | 718.97 | 165637.84 | 449556.46 |
| 54 | 60+94.00 CR-NE | 0.00' | 718.86 | 165638.17 | 449550.45 |
| 55 | 60+96.96 CR-NE | 0.00' RT | 718.82 | 165638.52 | 449547.51 |
| 56 | 60+99.47 CR-NE | 2.01' RT | 718.73 | 165640.88 | 449545.37 |
| 60 | 61+52.14 CR-NE | 2.00' RT | 718.28 | 165677.07 | 449518.93 |
| 61 | 61+57.12 CR-NE | 0.00' | 718.30 | 165682.04 | 449516.90 |
| 62 | 61+63.12 CR-NE | 0.00' | 718.23 | 165688.04 | 449516.87 |
| 63 | 61+68.12 CR-NE | 2.00' RT | 718.08 | 165693.05 | 449518.84 |
| 64 | 60+82.86 CR-NE | 2.50' RT | 719.50 | 165640.46 | 449561.43 |
| 65 | 60+88.06 CR-NE | 2.50' RT | 718.89 | 165640.34 | 449556.41 |
| 66 | 60+94.30 CR-NE | 2.50' RT | 718.78 | 165640.69 | 449550.40 |
| 67 | 60+99.48 CR-NE | 2.50' RT | 719.19 | 165641.37 | 449545.44 |
| 68 | 61+52.14 CR-NE | 2.50' RT | 718.77 | 165677.08 | 449519.43 |
| 69 | 61+57.12 CR-NE | 2.50' RT | 718.22 | 165682.05 | 449519.40 |
| 70 | 61+63.12 CR-NE | 2.50' RT | 718.15 | 165688.05 | 449519.37 |
| 71 | 61+68.12 CR-NE | 2.50' RT | 718.57 | 165693.05 | 449519.34 |
| 72 | 61+63.12 CR-NE | 6.93' RT | 718.46 | 165688.07 | 449523.80 |
| 73 | 61+57.12 CR-NE | 6.93' RT | 718.53 | 165682.07 | 449523.83 |
| 74 | 61+63.12 CR-NE | 11.93' RT | 718.53 | 165688.10 | 449528.80 |
| 75 | 61+57.12 CR-NE | 11.93' RT | 718.61 | 165682.10 | 449528.83 |
| 76 | 60+95.74 CR-NE | 12.37' RT | 719.48 | 165650.64 | 449550.20 |
| 77 | 60+88.48 CR-NE | 12.90' RT | 719.54 | 165650.75 | 449556.20 |
| 78 | 60+96.64 CR-NE | 17.32' RT | 719.55 | 165655.64 | 449550.11 |
| 79 | 60+88.73 CR-NE | 17.90' RT | 719.61 | 165655.75 | 449556.11 |
| 82 | 60+94.24 CR-NE | 2.00' RT | 718.78 | 165640.18 | 449550.41 |
| 83 | 60+88.05 CR-NE | 2.00' RT | 718.89 | 165639.84 | 449556.42 |
| 84 | 61+57.12 CR-NE | 2.00' RT | 718.22 | 165682.05 | 449518.90 |
| 85 | 61+63.12 CR-NE | 2.00' RT | 718.15 | 165688.05 | 449518.87 |

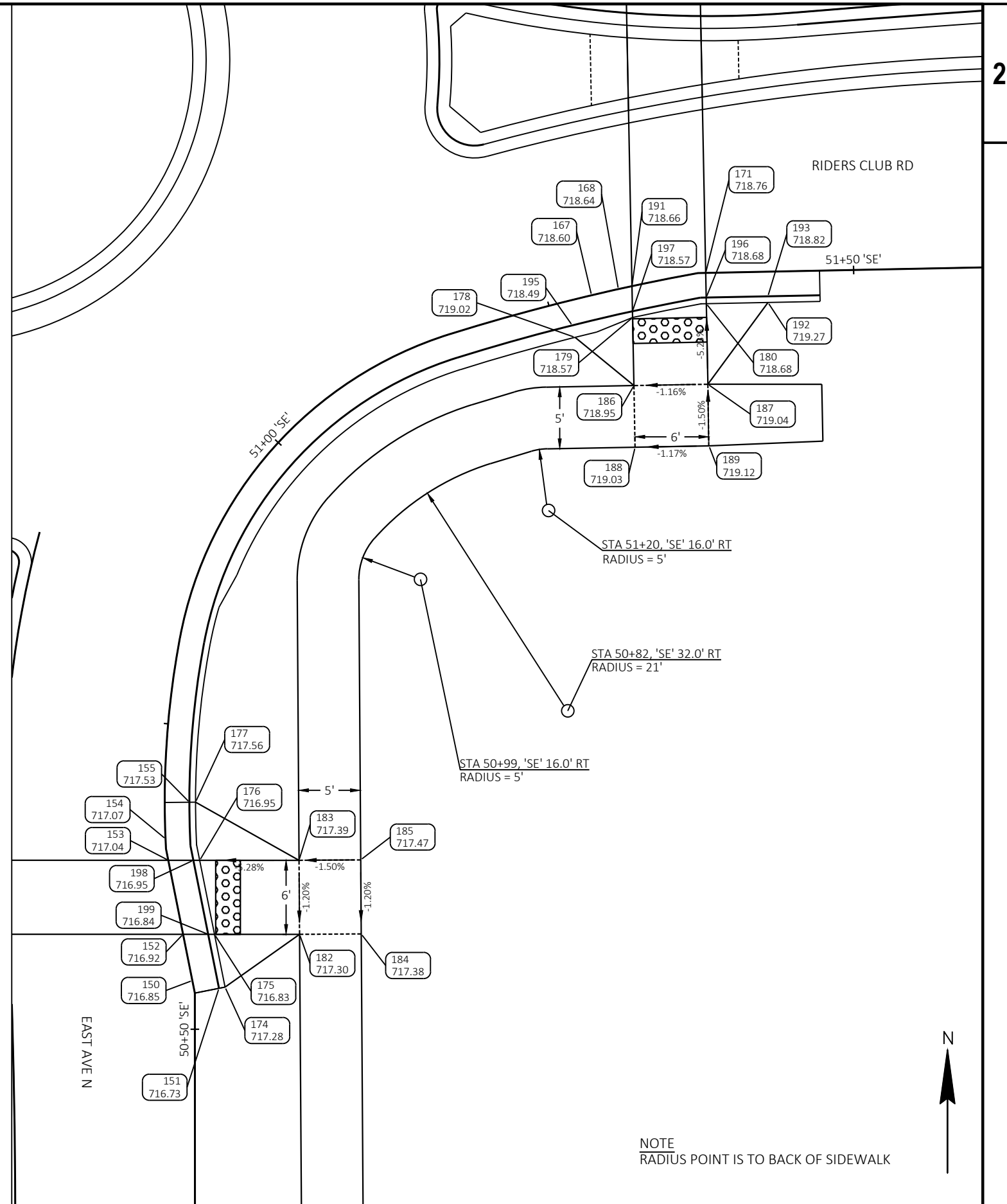




| CR-SW | | | | | |
|--------------|----------------|-----------|-----------|-----------|-----------|
| POINT NUMBER | STATION | OFFSET | ELEVATION | NORTHING | EASTING |
| 100 | 81+06.73 CR-SW | 2.01' RT | 717.56 | 165602.38 | 449429.50 |
| 101 | 81+11.84 CR-SW | 0.00' RT | 717.66 | 165604.53 | 449434.47 |
| 103 | 81+17.85 CR-SW | 0.00' RT | 717.67 | 165604.24 | 449440.48 |
| 105 | 81+23.31 CR-SW | 2.01' RT | 717.61 | 165601.56 | 449445.56 |
| 114 | 81+82.32 CR-SW | 2.00' RT | 717.02 | 165561.47 | 449475.55 |
| 116 | 81+87.38 CR-SW | 0.00' | 716.97 | 165556.42 | 449477.55 |
| 117 | 81+93.38 CR-SW | 0.00' | 716.86 | 165550.42 | 449477.55 |
| 119 | 81+98.37 CR-SW | 2.00' RT | 716.64 | 165545.42 | 449475.55 |
| 120 | 81+06.71 CR-SW | 2.49' RT | 718.02 | 165601.90 | 449429.51 |
| 121 | 81+11.89 CR-SW | 2.50' RT | 717.58 | 165602.03 | 449434.51 |
| 122 | 81+18.13 CR-SW | 2.50' RT | 717.60 | 165601.73 | 449440.52 |
| 123 | 81+23.33 CR-SW | 2.50' RT | 718.07 | 165601.07 | 449445.50 |
| 124 | 81+82.32 CR-SW | 2.50' RT | 717.49 | 165561.47 | 449475.05 |
| 125 | 81+87.38 CR-SW | 2.50' RT | 716.89 | 165556.42 | 449475.05 |
| 126 | 81+93.38 CR-SW | 2.50' RT | 716.78 | 165550.42 | 449475.05 |
| 127 | 81+98.38 CR-SW | 2.50' RT | 717.11 | 165545.42 | 449475.05 |
| 128 | 81+12.14 CR-SW | 11.40' RT | 718.11 | 165593.12 | 449434.65 |
| 129 | 81+19.23 CR-SW | 10.95' RT | 718.20 | 165593.21 | 449440.65 |
| 130 | 81+12.31 CR-SW | 16.40' RT | 718.19 | 165588.13 | 449434.73 |
| 131 | 81+20.02 CR-SW | 15.91' RT | 718.27 | 165588.21 | 449440.73 |
| 132 | 81+87.38 CR-SW | 8.85' RT | 717.29 | 165556.42 | 449468.70 |
| 133 | 81+93.38 CR-SW | 8.83' RT | 717.23 | 165550.42 | 449468.72 |
| 134 | 81+87.39 CR-SW | 13.85' RT | 717.36 | 165556.41 | 449463.70 |
| 135 | 81+93.39 CR-SW | 13.83' RT | 717.30 | 165550.41 | 449463.72 |
| 200 | 81+87.38 CR-SW | 2.00' RT | 716.89 | 165556.42 | 449475.55 |
| 201 | 81+93.38 CR-SW | 2.00' RT | 716.78 | 165550.42 | 449475.55 |
| 202 | 81+11.88 CR-SW | 2.00' RT | 717.58 | 165602.53 | 449434.50 |
| 203 | 81+18.07 CR-SW | 2.00' RT | 717.59 | 165602.23 | 449440.51 |

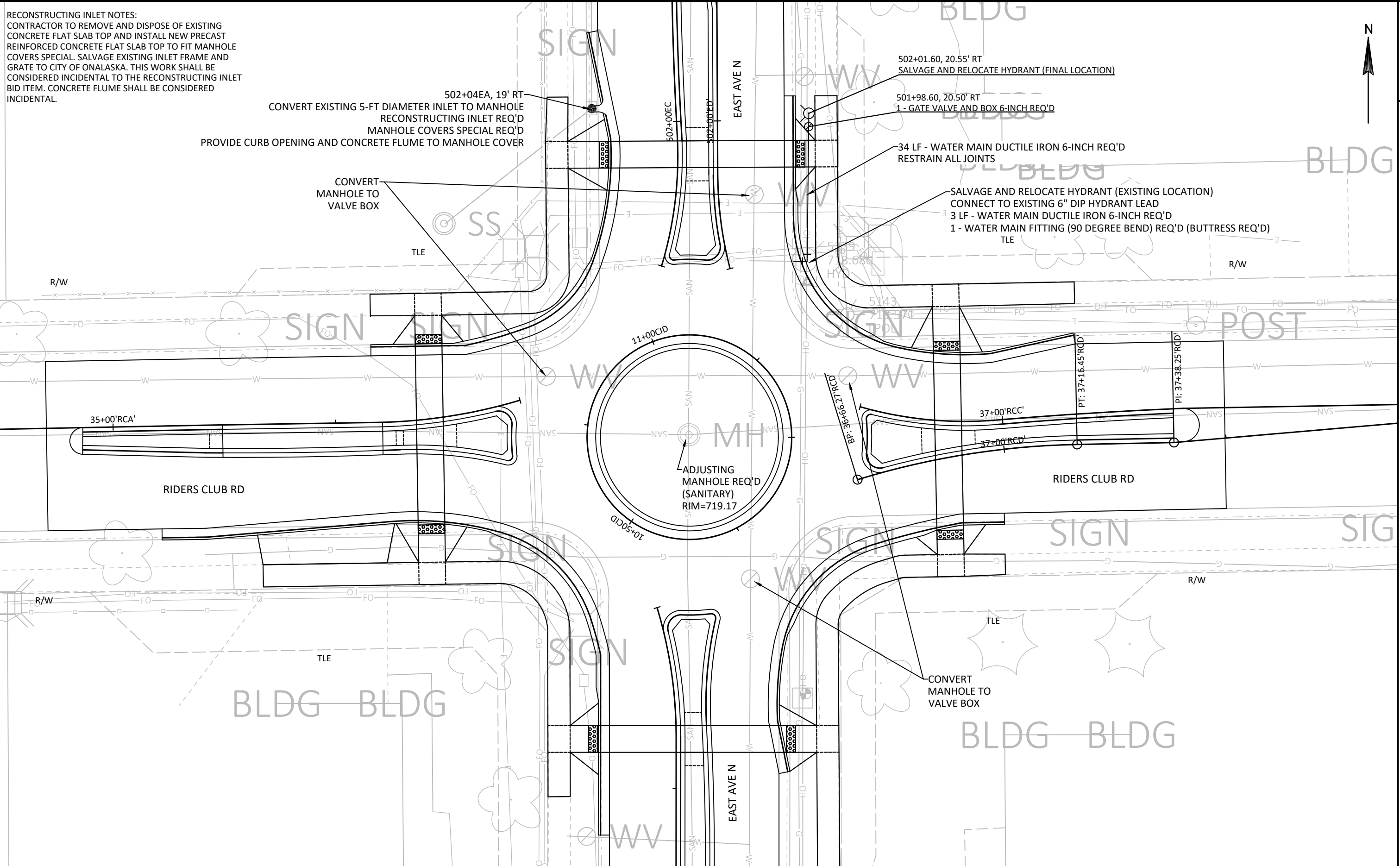
NOTE
RADIUS POINT IS TO BACK OF SIDEWALK

| CR-SE | | | | | |
|--------------|----------------|-----------|-----------|-----------|-----------|
| POINT NUMBER | STATION | OFFSET | ELEVATION | NORTHING | EASTING |
| 150 | 50+53.96 CR-SE | 0.00' | 716.85 | 165546.66 | 449515.49 |
| 151 | 50+52.93 CR-SE | 2.00' RT | 716.73 | 165546.03 | 449517.65 |
| 152 | 50+57.79 CR-SE | 0.00' | 716.92 | 165550.41 | 449514.74 |
| 153 | 50+63.91 CR-SE | 0.00' | 717.04 | 165556.41 | 449513.54 |
| 154 | 50+65.67 CR-SE | 0.00' | 717.07 | 165558.15 | 449513.31 |
| 155 | 50+68.64 CR-SE | 2.01' RT | 717.53 | 165561.12 | 449515.26 |
| 161 | 50+92.50 CR-SE | 0.01' RT | 719.16 | 165584.23 | 449518.06 |
| 162 | 51+01.00 CR-SE | 0.01' RT | 719.16 | 165590.99 | 449523.16 |
| 163 | 51+09.50 CR-SE | 0.01' LT | 719.16 | 165596.19 | 449529.85 |
| 167 | 51+28.56 CR-SE | 0.00' RT | 718.60 | 165602.21 | 449547.86 |
| 168 | 51+30.80 CR-SE | 0.00' | 718.64 | 165602.71 | 449550.04 |
| 171 | 51+38.00 CR-SE | 0.00' | 718.76 | 165604.03 | 449557.12 |
| 174 | 50+52.93 CR-SE | 2.50' RT | 717.28 | 165546.13 | 449518.14 |
| 175 | 50+57.29 CR-SE | 2.50' RT | 716.83 | 165550.41 | 449517.29 |
| 176 | 50+63.41 CR-SE | 2.50' RT | 716.95 | 165556.41 | 449516.09 |
| 177 | 50+68.64 CR-SE | 2.50' RT | 717.56 | 165561.13 | 449515.75 |
| 178 | 51+26.43 CR-SE | 3.00' RT | 719.02 | 165598.80 | 449546.52 |
| 179 | 51+31.43 CR-SE | 2.50' RT | 718.57 | 165600.41 | 449551.19 |
| 180 | 51+38.00 CR-SE | 2.50' RT | 718.68 | 165601.53 | 449557.17 |
| 182 | 50+55.94 CR-SE | 9.26' RT | 717.30 | 165550.41 | 449524.19 |
| 183 | 50+61.83 CR-SE | 10.40' RT | 717.39 | 165556.41 | 449524.15 |
| 184 | 50+54.99 CR-SE | 14.17' RT | 717.38 | 165550.44 | 449529.19 |
| 185 | 50+60.88 CR-SE | 15.31' RT | 717.47 | 165556.44 | 449529.15 |
| 186 | 51+30.30 CR-SE | 7.91' RT | 718.95 | 165594.89 | 449551.30 |
| 187 | 51+36.52 CR-SE | 9.01' RT | 719.04 | 165595.00 | 449557.30 |
| 188 | 51+29.21 CR-SE | 12.80' RT | 719.03 | 165589.89 | 449551.39 |
| 189 | 51+35.62 CR-SE | 13.94' RT | 719.12 | 165590.00 | 449557.39 |
| 191 | 51+31.92 CR-SE | 0.00' RT | 718.66 | 165602.95 | 449551.14 |
| 192 | 51+43.00 CR-SE | 2.50' RT | 719.27 | 165601.63 | 449562.17 |
| 193 | 51+43.00 CR-SE | 2.00' RT | 718.82 | 165602.13 | 449562.16 |
| 195 | 51+26.45 CR-SE | 2.00' RT | 718.49 | 165599.77 | 449546.30 |
| 196 | 51+38.00 CR-SE | 2.00' RT | 718.68 | 165602.03 | 449557.16 |
| 197 | 51+31.53 CR-SE | 2.00' RT | 718.57 | 165600.92 | 449551.18 |
| 198 | 50+63.51 CR-SE | 2.00' RT | 716.95 | 165556.41 | 449515.58 |
| 199 | 50+57.39 CR-SE | 2.00' RT | 716.84 | 165550.41 | 449516.78 |



NOTE
RADIUS POINT IS TO BACK OF SIDEWALK

RECONSTRUCTING INLET NOTES:
 CONTRACTOR TO REMOVE AND DISPOSE OF EXISTING CONCRETE FLAT SLAB TOP AND INSTALL NEW PRECAST REINFORCED CONCRETE FLAT SLAB TOP TO FIT MANHOLE COVERS SPECIAL. SALVAGE EXISTING INLET FRAME AND GRATE TO CITY OF ONALASKA. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE RECONSTRUCTING INLET BID ITEM. CONCRETE FLUME SHALL BE CONSIDERED INCIDENTAL.



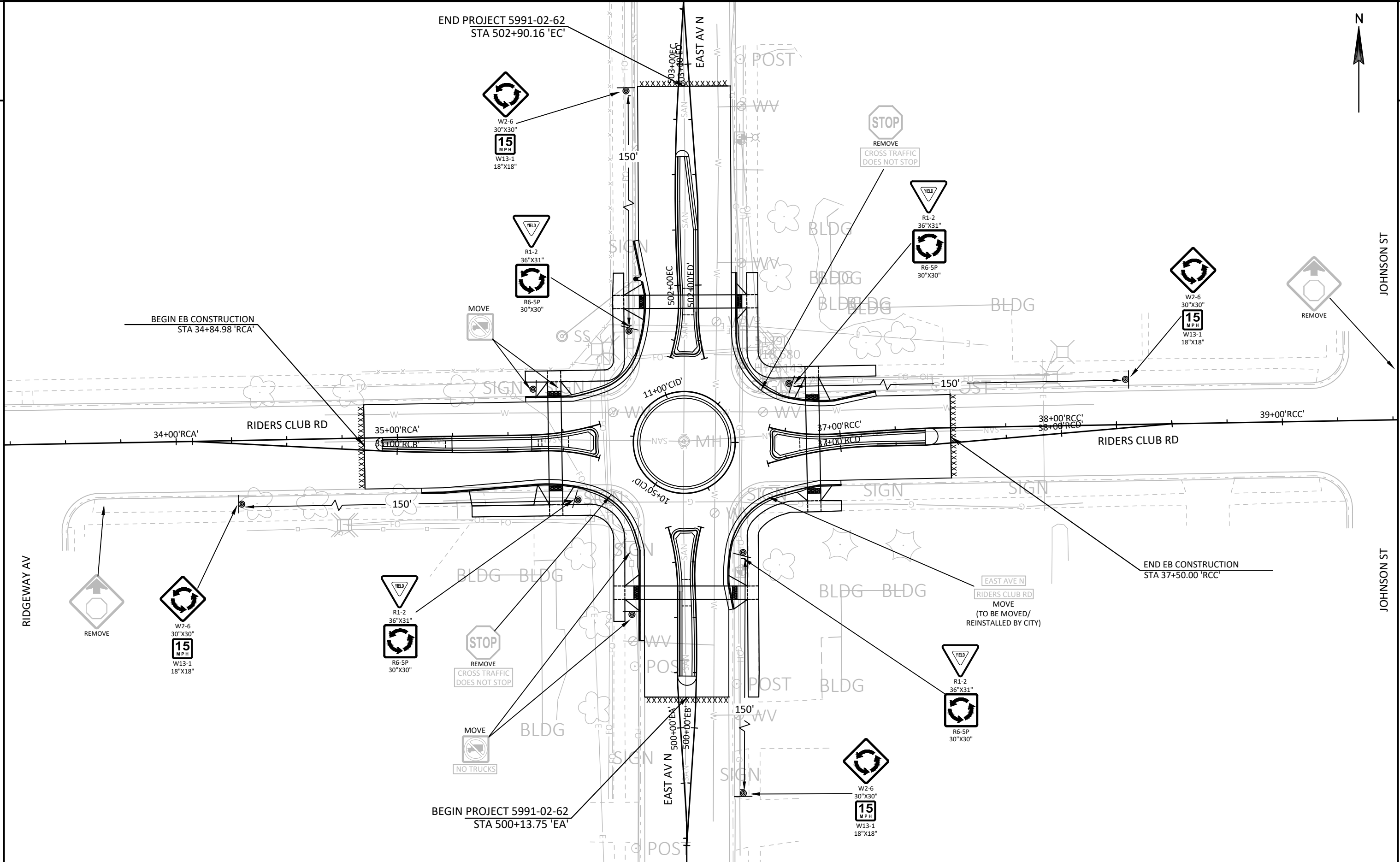
| | | | | | |
|------------------------|--------------------|-------------------|--------------|-------|---|
| PROJECT NO: 5991-02-62 | HWY: EAST AVENUE N | COUNTY: LA CROSSE | UTILITY PLAN | SHEET | E |
|------------------------|--------------------|-------------------|--------------|-------|---|

END PROJECT 5991-02-62
STA 502+90.16 'EC'

BEGIN EB CONSTRUCTION
STA 34+84.98 'RCA'

END EB CONSTRUCTION
STA 37+50.00 'RCC'

BEGIN PROJECT 5991-02-62
STA 500+13.75 'EA'



| | | | | | |
|------------------------|--------------------|-------------------|-------------------|-------|---|
| PROJECT NO: 5991-02-62 | HWY: EAST AVENUE N | COUNTY: LA CROSSE | PERMANENT SIGNING | SHEET | E |
|------------------------|--------------------|-------------------|-------------------|-------|---|

| LIGHTING ITEMS | | | |
|----------------|--------------|----------|-----------|
| POINT | STATION | OFFSET | ELEVATION |
| 1 | 35+62.36 RCA | 20.49' L | 718.24 |
| 2 | 35+51.25 RCA | 27.43' R | 718.18 |

| LIGHTING ITEMS | | | |
|----------------|--------------|----------|-----------|
| POINT | STATION | OFFSET | ELEVATION |
| 3 | 501+81.80 EC | 21.36' L | 718.79 |

| LIGHTING ITEMS | | | |
|----------------|--------------|----------|-----------|
| POINT | STATION | OFFSET | ELEVATION |
| 5 | 500+52.63 EA | 21.18' L | 717.27 |
| 6 | 500+53.21 EA | 30.14' R | 717.40 |

| LIGHTING ITEMS | | | |
|----------------|--------------|----------|-----------|
| POINT | STATION | OFFSET | ELEVATION |
| 4 | 36+96.21 RCC | 26.39' R | 719.48 |

UTILITY TO FURNISH SECONDARY SERVICE CONDUCTORS AND CONDUIT TO LIGHTING CONTROL CABINET

S.O.P.

NOTES:

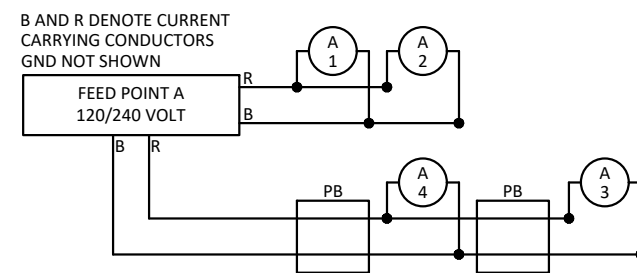
1. ALL CONDUIT SHALL BE 2" NMC SCHEDULE 40 UNLESS OTHERWISE NOTED.
2. ALL CONDUCTORS SHALL BE COPPER, TYPE XHHW-2, (2)#8 & (1)#8 GND.
3. PROVIDE (3)#12 STRANDED XHHW CONDUCTORS FROM POLE BASE TO LUMINAIRE.
4. ALL LIGHTING AND ELECTRICAL EQUIPMENT SHALL BE STAKED BY CONTRACTOR AND VERIFIED BY THE FIELD ENGINEER.
5. ELEVATION SHOWN ON TABLES REFERENCES FINISHED GRADE. CONTRACTOR TO ADJUST FOR TOP OF BASE.

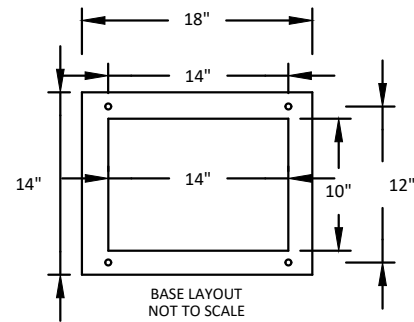
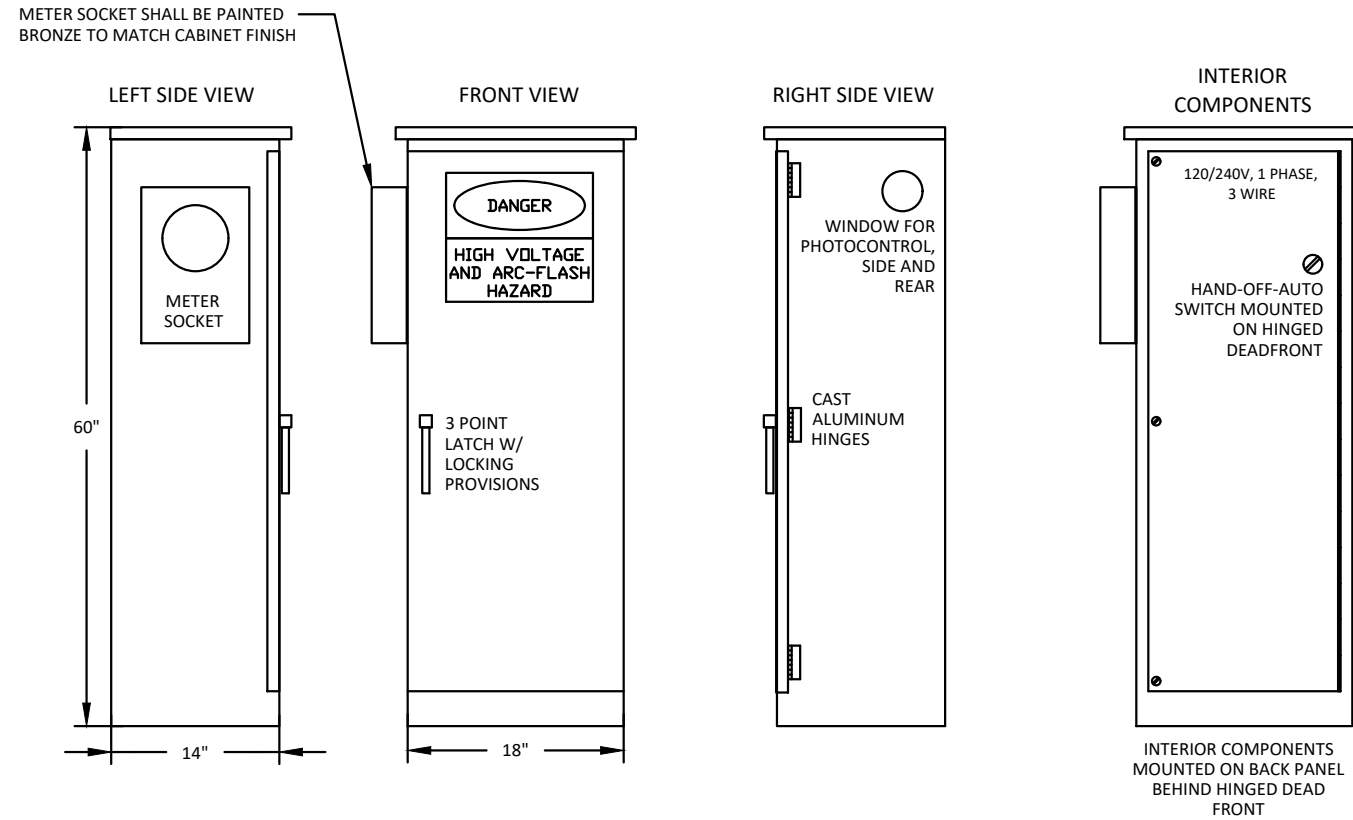
LEGEND

- LIGHTING UNIT
- UNDERGROUND WIRING IN CONDUIT
- LIGHTING CONTROL CABINET TYPE SPECIAL
- S.O.P.
- GROUND ROD
- CONDUIT STUB
- PULL BOX
- LIGHTING UNIT NUMBER

LEGEND

- PROPOSED LIGHTING UNIT NUMBER
- PULL BOX
- PROPOSED WIRE
- PROPOSED SPLICE IN POLE BASE

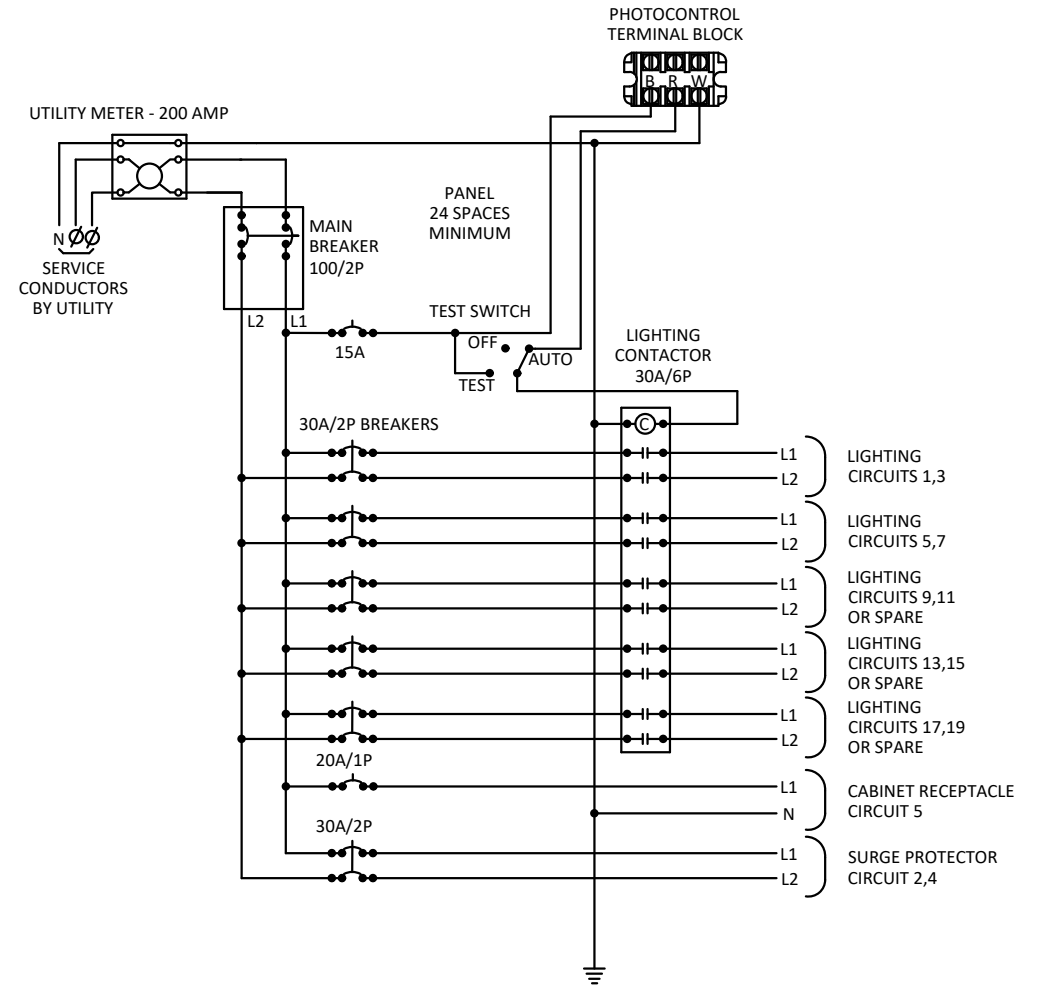




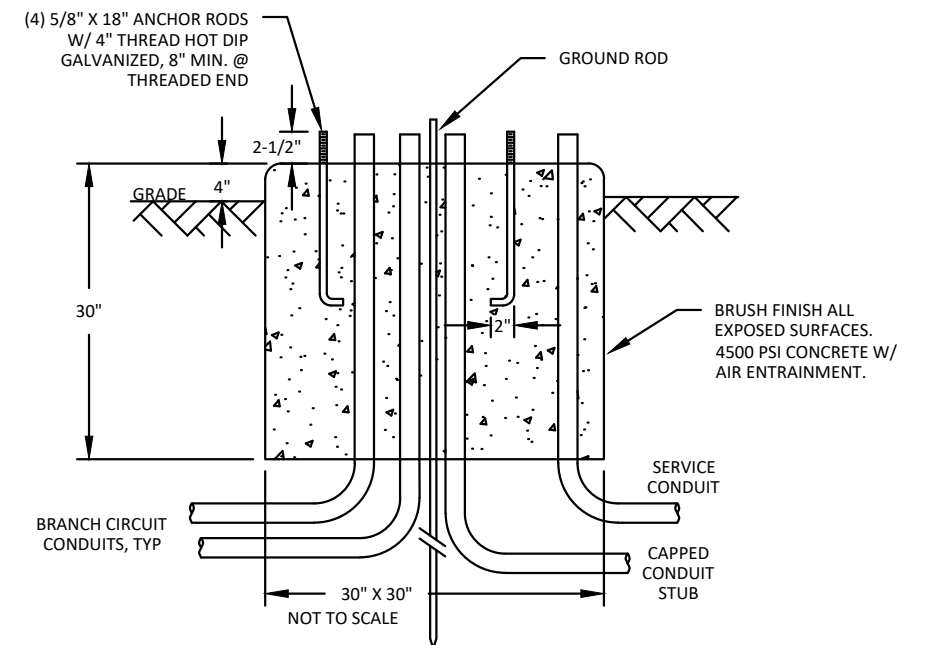
- CABINET CONSTRUCTION**
- NEMA 3R
 - INTERIOR COMPONENTS MOUNTED ON BACK PANEL BEHIND DEAD FRONT
 - 1/8" ANODIZED ALUMINUM (DURANODIC #313)
 - NEOPRENE GASKETED DOORS
 - STAINLESS STEEL HARDWARE
 - ETL LISTED IN ACCORDANCE WITH UL508A

LIGHTING CONTROL CABINET TYPE SPECIAL DETAILS

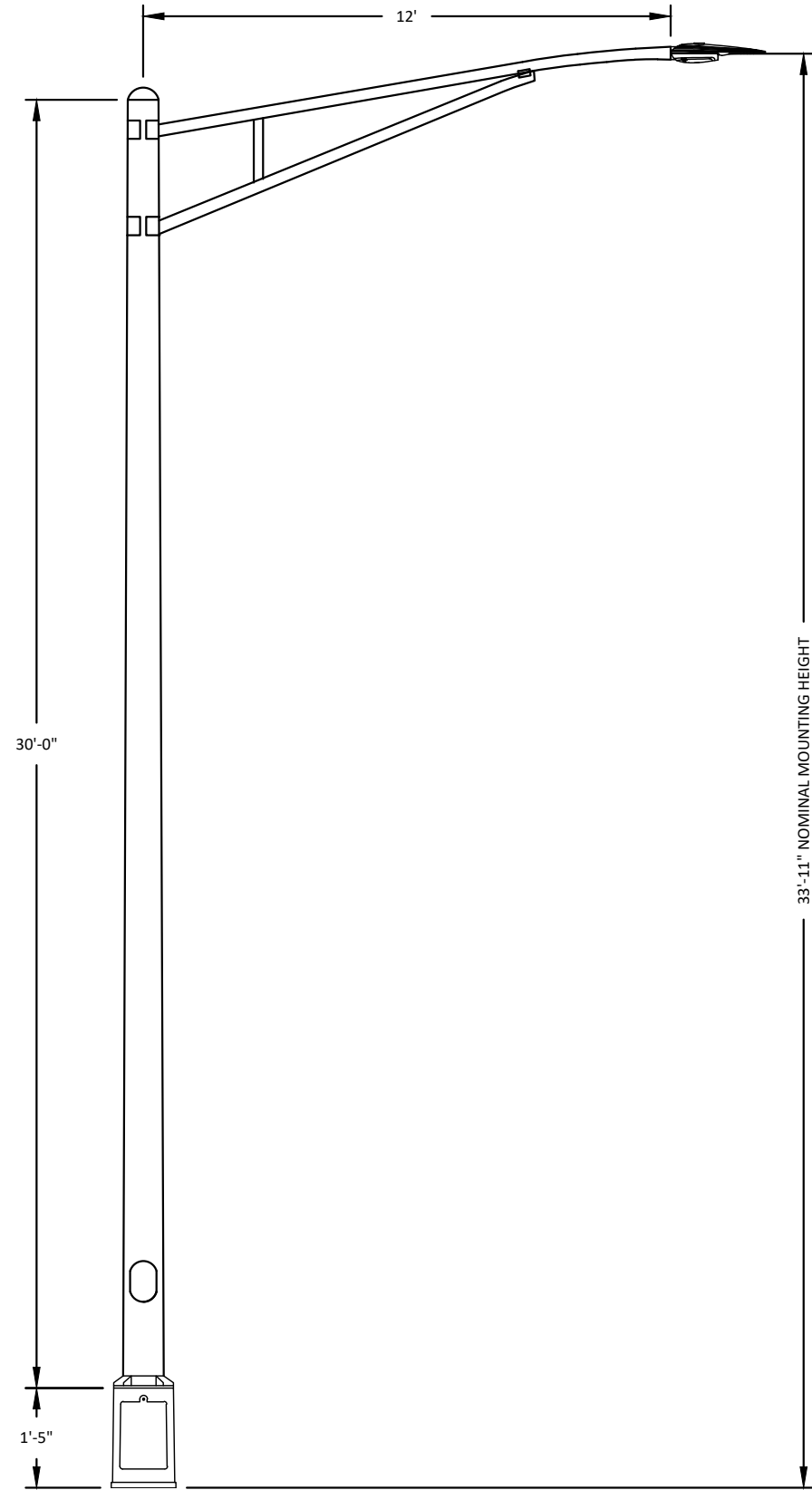
- LIGHTING CONTROL CABINET TYPE SPECIAL NOTES:**
- PROVIDE LEVER BYPASS PER UTILITY COMPANY REQUIREMENTS.
 - CIRCUIT BREAKERS SHALL BE 120/240 VOLT AC, 60Hz AND SHALL BE CLEARLY MARKED WITH THE "ON" AND "OFF" POSITIONS AND IDENTIFIED WITH THE LOAD WHICH IT IS CARRYING.
 - SHORT CIRCUIT RATING - 22,000 AIC SYMMETRICAL.
 - 3-POSITION SELECTOR SWITCH ALLEN BRADLEY #800T-J2A.
 - CIRCUIT CONTACTORS SHALL HAVE A 240 VOLT RATING, WITH 120 VOLT COIL.
 - PROVIDE 50KA SURGE PROTECTOR.
 - PROVIDE PANEL WITH DIMENSIONS AS REQUIRED TO FIT EQUIPMENT PROPOSED.
 - PROVIDE A 25-OHM GROUND AT CABINET AS PER NEC.
 - PROVIDE 20A WR-RATED GFCI RECEPTACLE MOUNTED TO CABINET DEAD-FRONT.
 - BOTH PHOTOCONTROL AND ITS SOCKET SHALL BE 3 TERMINAL, POLARIZED, TWIST-LOCK TYPE. IT SHALL BE EQUIPPED WITH A MOV TYPE LIGHTNING ARRESTER.
 - CABINET SHALL BE MANUFACTURED BY POVOLNY SPECIALTIES INC. 651.452.7335.
 - CONTRACTOR TO COORDINATE RIVERLAND ENERGY COOPERATIVE SERVICE CONNECTION(S) WITH TIM HOLTAN AT 608-323-3381.



LIGHTING CONTROL CABINET TYPE SPECIAL SCHEMATIC



CONCRETE CONTROL CABINET BASE TYPE SPECIAL DETAILS



LIGHTING UNIT DETAIL

EQUIPMENT SCHEDULE

| SYMBOL | DESCRIPTION | WATTAGE & CCT | LUMENS | MOUNTING | OPTICS | COLOR & FINISH | MANUFACTURER & SERIES # |
|--------|---|--------------------------|--------|---------------------------------|----------|----------------|--|
| ☼ | LIGHTING UNIT LED LUMINAIRE ON ROUND ALUMINUM POLE, 12-FOOT TRUSS ARM, AND TRANSFORMER BASE | 100 WATT LED 4000K | 13472 | 30' POLE ON LIGHT FOUNDATION | TYPE III | GRAY | TGS - WISDOT TYPE B LUMINAIRE: RWL2-M-100-40-U-P7S-3-WisDOT POLE- WISDOT TYPE 5 POLE, 12-FOOT TRUSS ARM, TRANSFORMER BASE |

PAVEMENT MARKING LEGEND

- 1. MARKING LINE GROOVED EPOXY 4-INCH - YELLOW
- 2. MARKING LINE GROOVED EPOXY 4-INCH - WHITE
- 3. MARKING LINE GROOVED EPOXY 4-INCH - WHITE, 3-FT LINE, 9-FT GAP
- 4. MARKING CROSSWALK GROOVED EPOXY TRANSVERSE LINE 6-INCH - WHITE
- 5. MARKING LINE GROOVED EPOXY 8-INCH - WHITE
- 6. MARKING DOTTED EXTENSION GROOVED EPOXY 18-INCH - WHITE, 2-ft SEGMENT, 2-FT GAP
- 7. MARKING DIAGONAL GROOVED EPOXY 12-INCH - YELLOW
- 8. MARKING ARROW GROOVED EPOXY - WHITE
- 9. MARKING SYMBOL GROOVED EPOXY - WHITE
- 10. MARKING ISLAND NOSE EPOXY - YELLOW
- 11. MARKING REMOVAL LINE WATER BLASTING 4-INCH



STA 503+29'EC'

503+00'EC'

EAST AV N

501+00'EC'

11+00

EAST AV N

500+00'EA'

500+00'EA'

STA 499+47'EA'

REMOVE EXISTING CENTERLINE SKIPS

REMOVE EXISTING CENTERLINE SKIPS

34+00'RCA'

RIDERS CLUB RD

35+00'RCA'

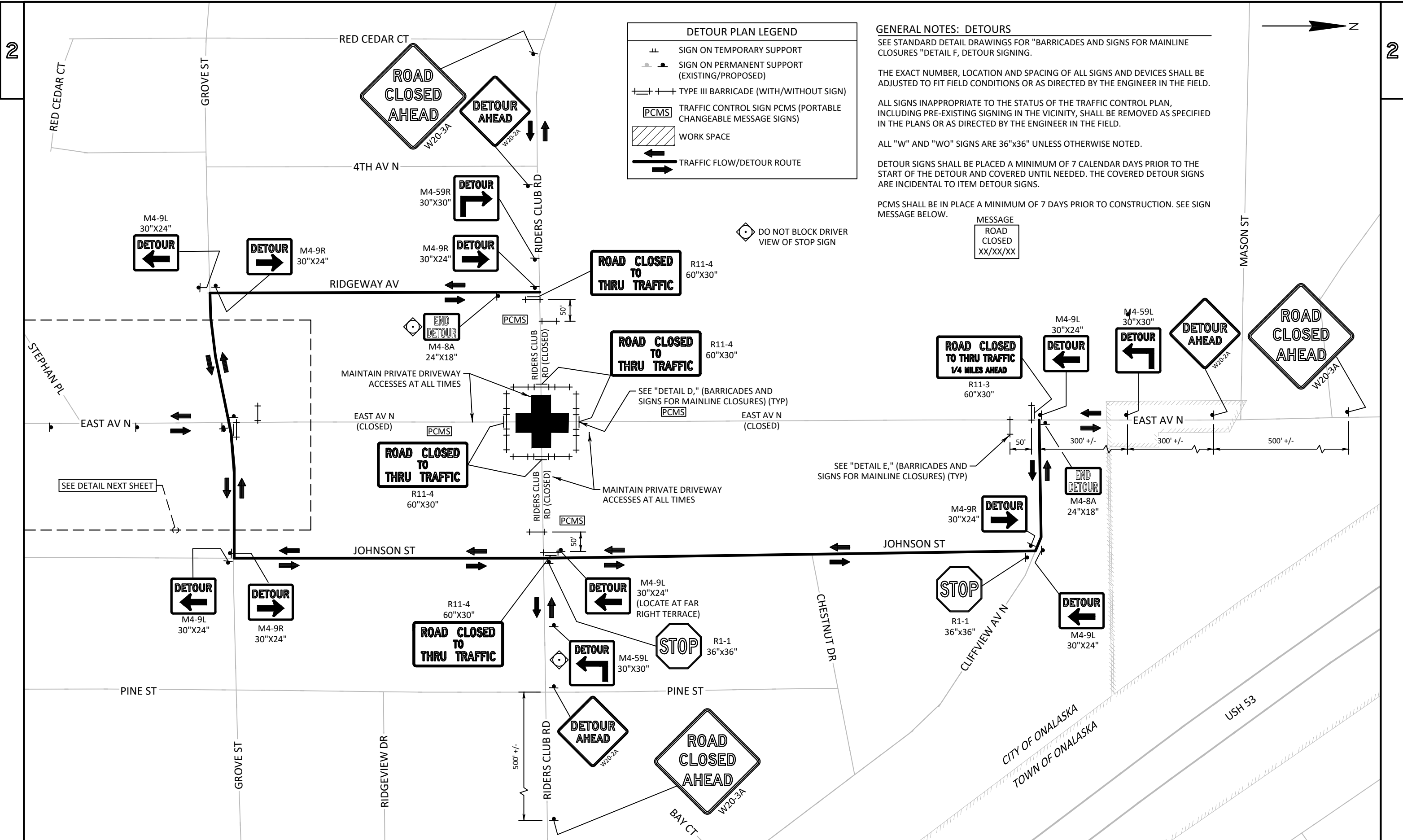
37+00'RCC'

RIDERS CLUB RD

38+00'RCC'

STA 38+50'RCC'

39+00'RCC'



| DETOUR PLAN LEGEND | |
|--------------------|---|
| | SIGN ON TEMPORARY SUPPORT |
| | SIGN ON PERMANENT SUPPORT (EXISTING/PROPOSED) |
| | TYPE III BARRICADE (WITH/WITHOUT SIGN) |
| | TRAFFIC CONTROL SIGN PCMS (PORTABLE CHANGEABLE MESSAGE SIGNS) |
| | WORK SPACE |
| | TRAFFIC FLOW/DETOUR ROUTE |

GENERAL NOTES: DETOURS
 SEE STANDARD DETAIL DRAWINGS FOR "BARRICADES AND SIGNS FOR MAINLINE CLOSURES" DETAIL F, DETOUR SIGNING.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS OR AS DIRECTED BY THE ENGINEER IN THE FIELD.

ALL SIGNS INAPPROPRIATE TO THE STATUS OF THE TRAFFIC CONTROL PLAN, INCLUDING PRE-EXISTING SIGNING IN THE VICINITY, SHALL BE REMOVED AS SPECIFIED IN THE PLANS OR AS DIRECTED BY THE ENGINEER IN THE FIELD.

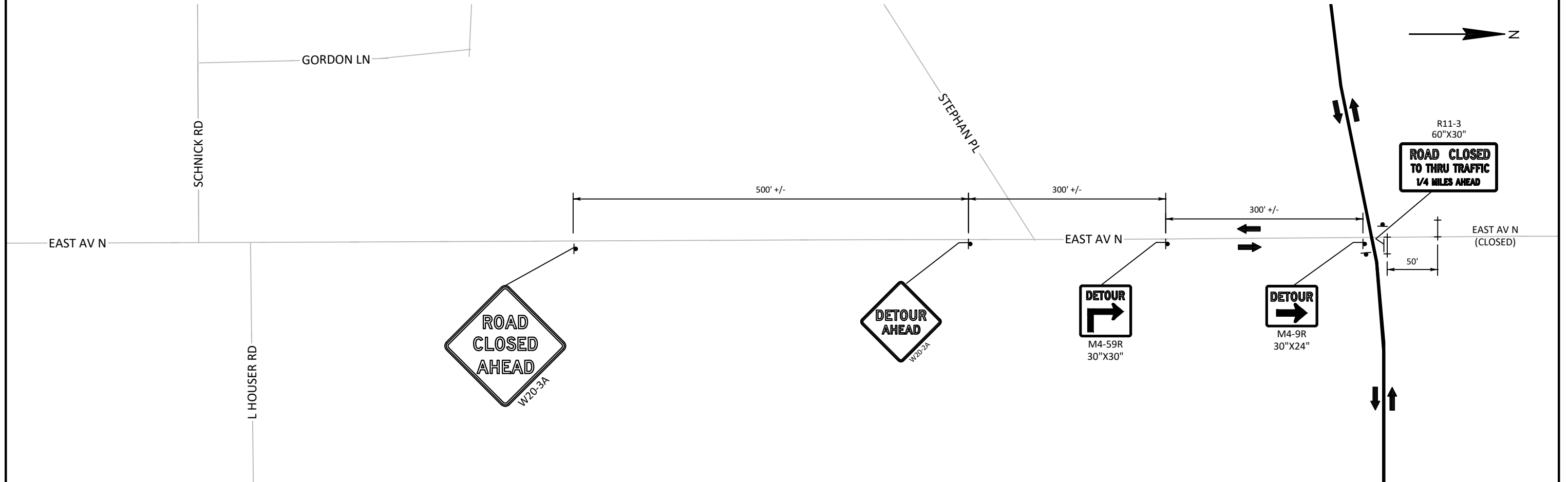
ALL "W" AND "WO" SIGNS ARE 36"x36" UNLESS OTHERWISE NOTED.

DETOUR SIGNS SHALL BE PLACED A MINIMUM OF 7 CALENDAR DAYS PRIOR TO THE START OF THE DETOUR AND COVERED UNTIL NEEDED. THE COVERED DETOUR SIGNS ARE INCIDENTAL TO ITEM DETOUR SIGNS.

PCMS SHALL BE IN PLACE A MINIMUM OF 7 DAYS PRIOR TO CONSTRUCTION. SEE SIGN MESSAGE BELOW.







MESSAGE
 ROAD
 CLOSED
 XX/XX/XX

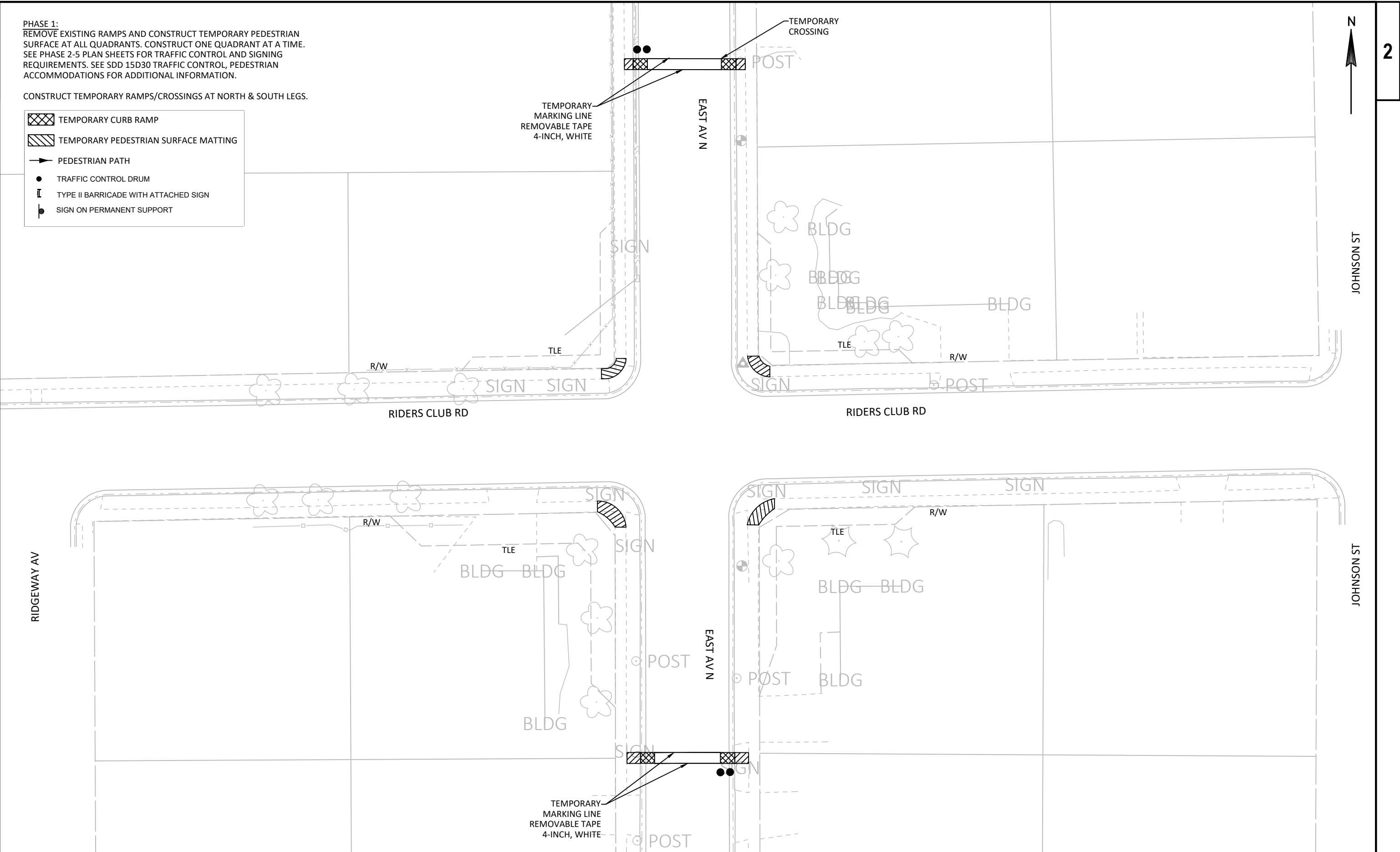
| DETOUR PLAN LEGEND | |
|--------------------|---|
| | SIGN ON TEMPORARY SUPPORT |
| | SIGN ON PERMANENT SUPPORT (EXISTING/PROPOSED) |
| | TYPE III BARRICADE (WITH/WITHOUT SIGN) |
| | TRAFFIC CONTROL SIGN PCMS (PORTABLE CHANGEABLE MESSAGE SIGNS) |
| | WORK SPACE |
| | TRAFFIC FLOW/DETOUR ROUTE |



PHASE 1:
 REMOVE EXISTING RAMPS AND CONSTRUCT TEMPORARY PEDESTRIAN SURFACE AT ALL QUADRANTS. CONSTRUCT ONE QUADRANT AT A TIME. SEE PHASE 2-5 PLAN SHEETS FOR TRAFFIC CONTROL AND SIGNING REQUIREMENTS. SEE SDD 15D30 TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATIONS FOR ADDITIONAL INFORMATION.







CONSTRUCT TEMPORARY RAMPS/CROSSINGS AT NORTH & SOUTH LEGS.

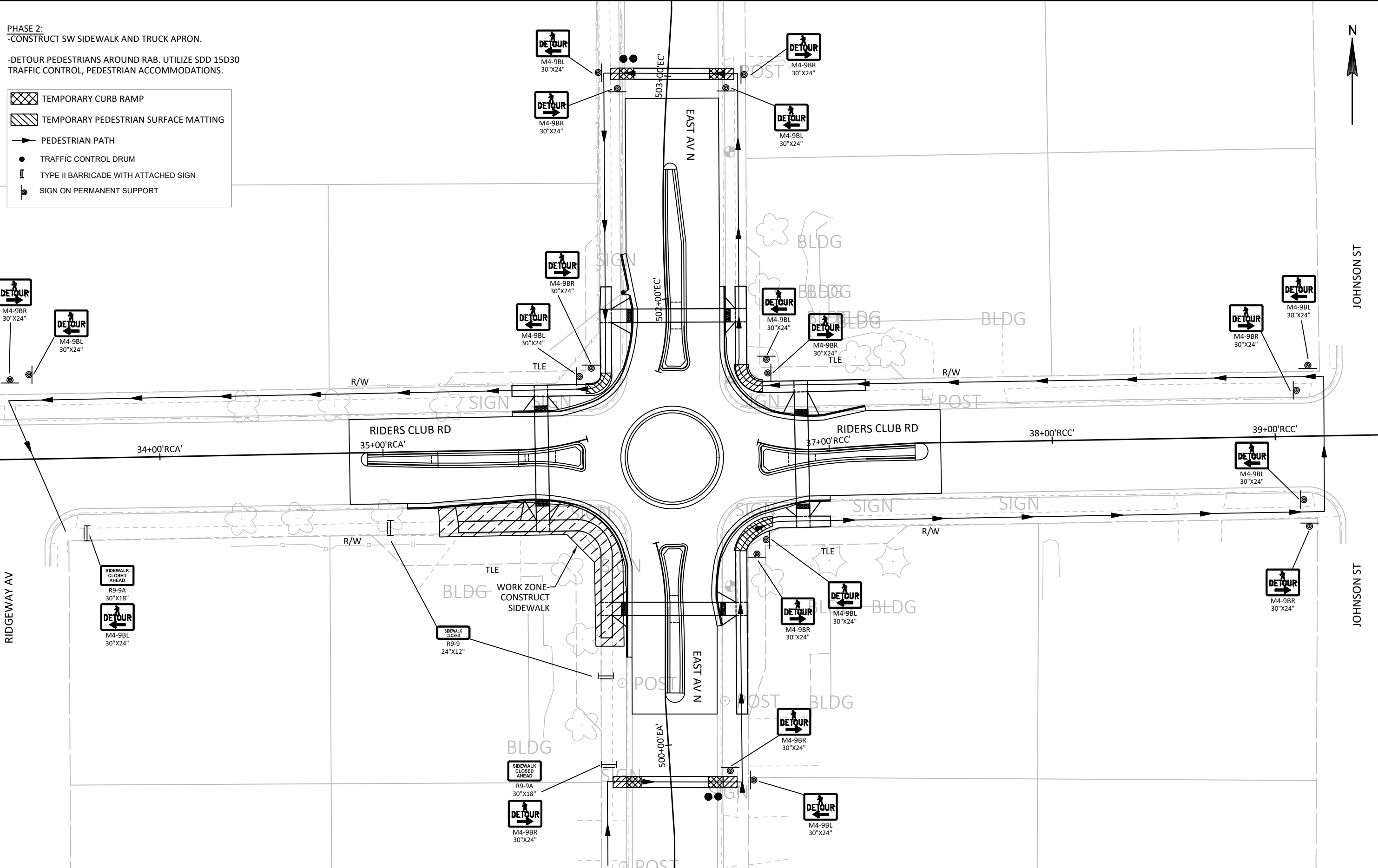
-  TEMPORARY CURB RAMP
-  TEMPORARY PEDESTRIAN SURFACE MATTING
-  PEDESTRIAN PATH
-  TRAFFIC CONTROL DRUM
-  TYPE II BARRICADE WITH ATTACHED SIGN
-  SIGN ON PERMANENT SUPPORT



PHASE 2:
-CONSTRUCT SW SIDEWALK AND TRUCK APRON.







-DETOUR PEDESTRIANS AROUND RAB. UTILIZE SDD 15D30
TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATIONS.

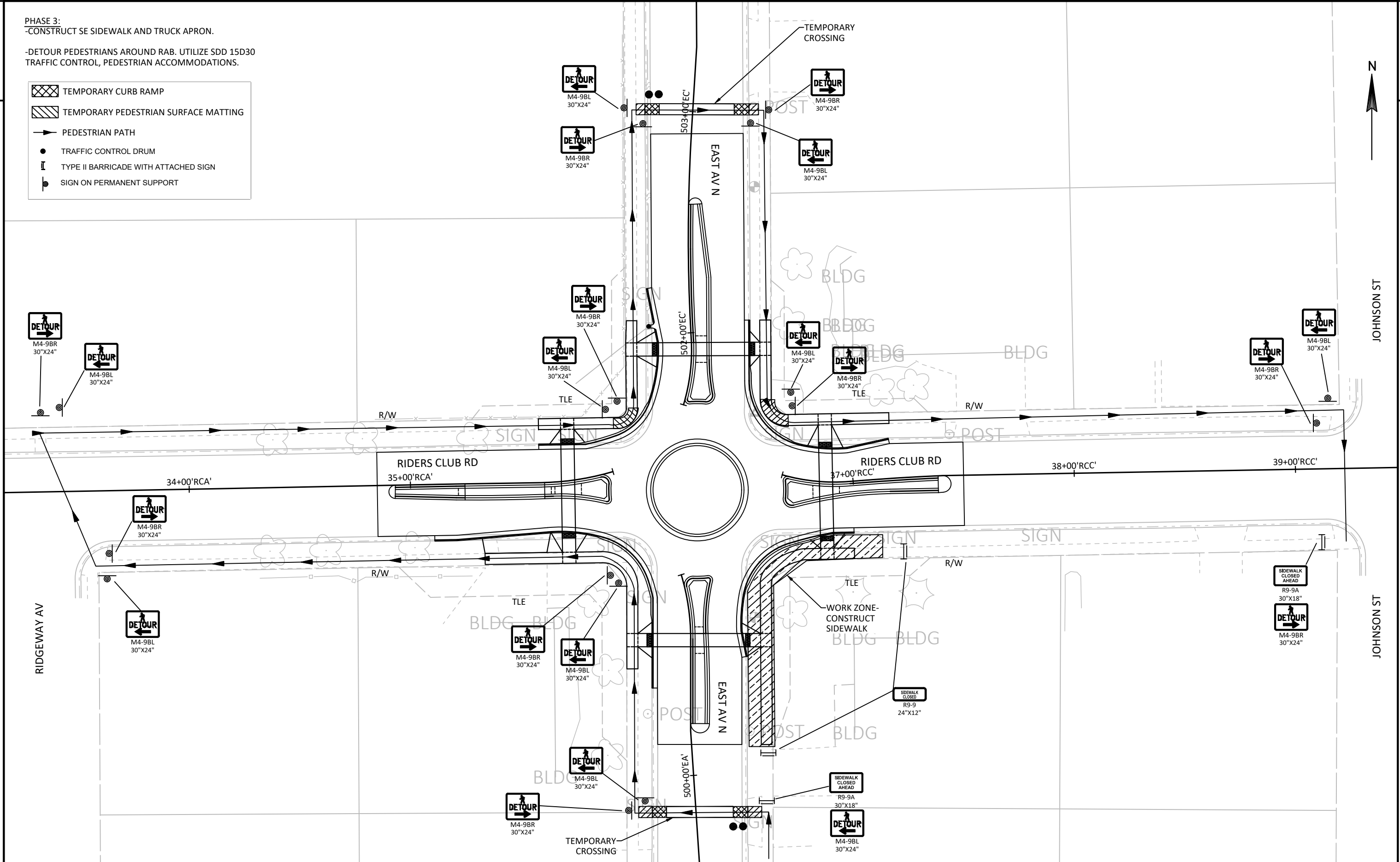
-  TEMPORARY CURB RAMP
-  TEMPORARY PEDESTRIAN SURFACE MATTING
-  PEDESTRIAN PATH
-  TRAFFIC CONTROL DRUM
-  TYPE II BARRICADE WITH ATTACHED SIGN
-  SIGN ON PERMANENT SUPPORT



PHASE 3:
-CONSTRUCT SE SIDEWALK AND TRUCK APRON.







-DETOUR PEDESTRIANS AROUND RAB. UTILIZE SDD 15D30
TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATIONS.

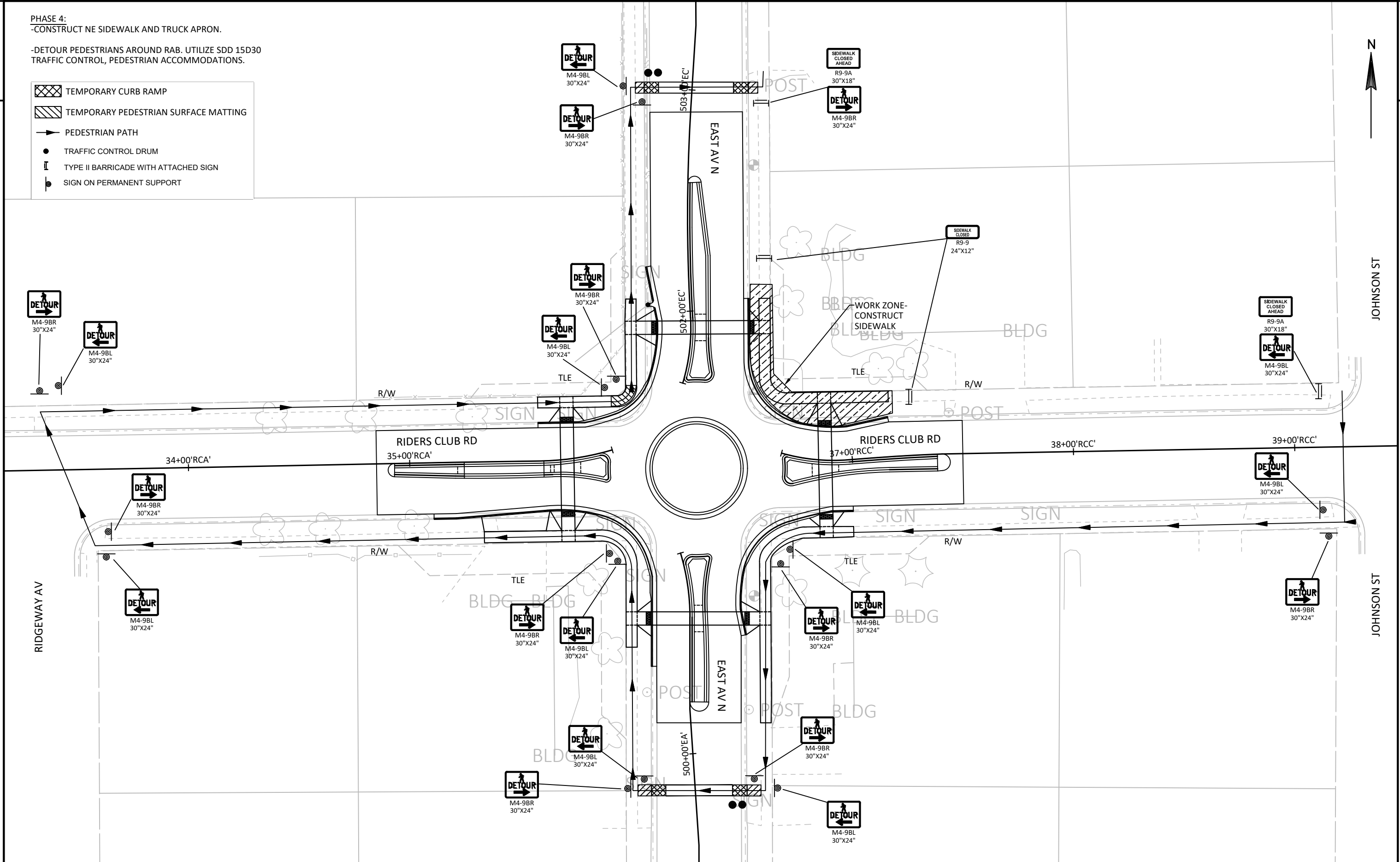
-  TEMPORARY CURB RAMP
-  TEMPORARY PEDESTRIAN SURFACE MATTING
-  PEDESTRIAN PATH
-  TRAFFIC CONTROL DRUM
-  TYPE II BARRICADE WITH ATTACHED SIGN
-  SIGN ON PERMANENT SUPPORT



PHASE 4:
-CONSTRUCT NE SIDEWALK AND TRUCK APRON.







-DETOUR PEDESTRIANS AROUND RAB. UTILIZE SDD 15D30
TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATIONS.

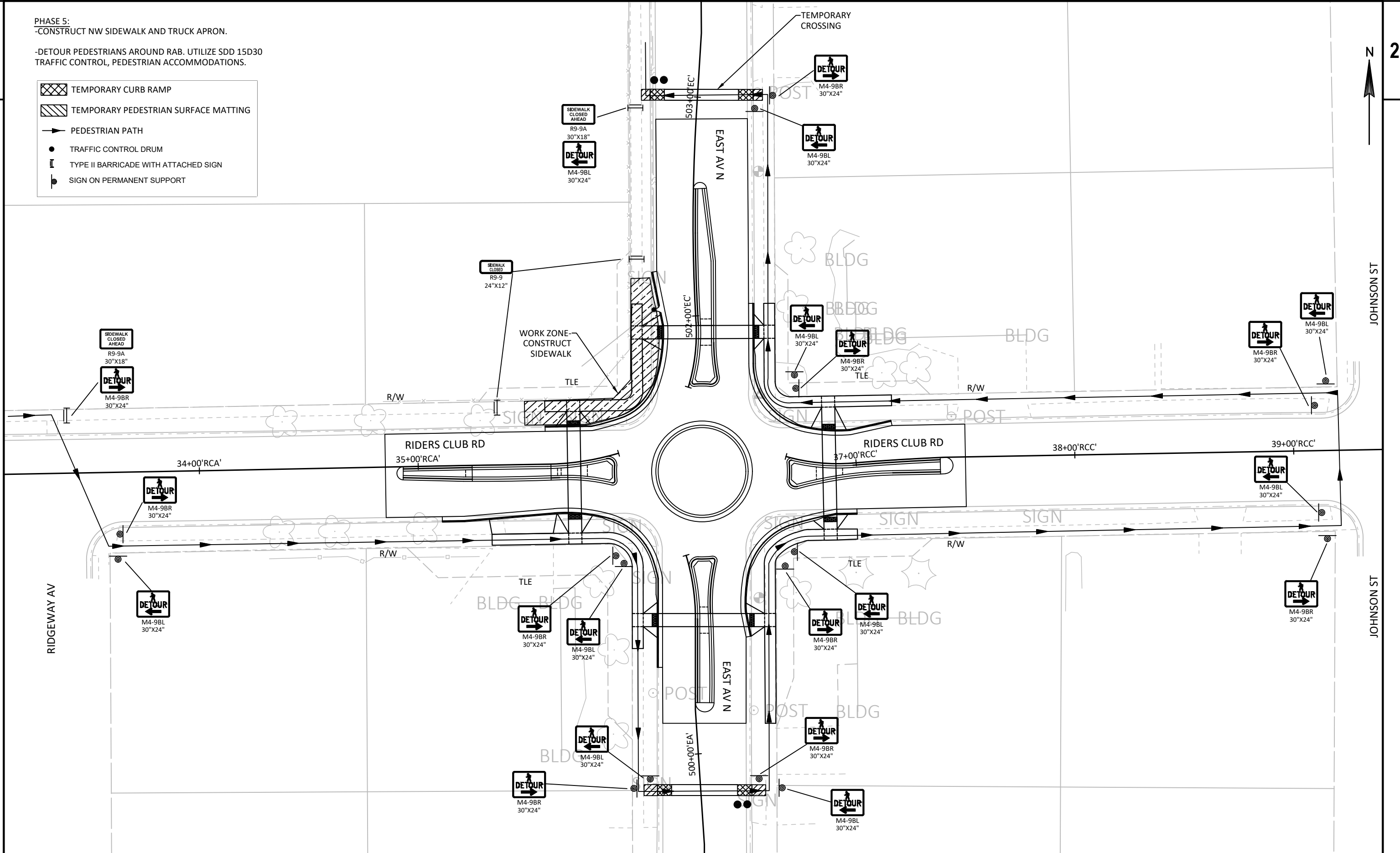
-  TEMPORARY CURB RAMP
-  TEMPORARY PEDESTRIAN SURFACE MATTING
-  PEDESTRIAN PATH
-  TRAFFIC CONTROL DRUM
-  TYPE II BARRICADE WITH ATTACHED SIGN
-  SIGN ON PERMANENT SUPPORT

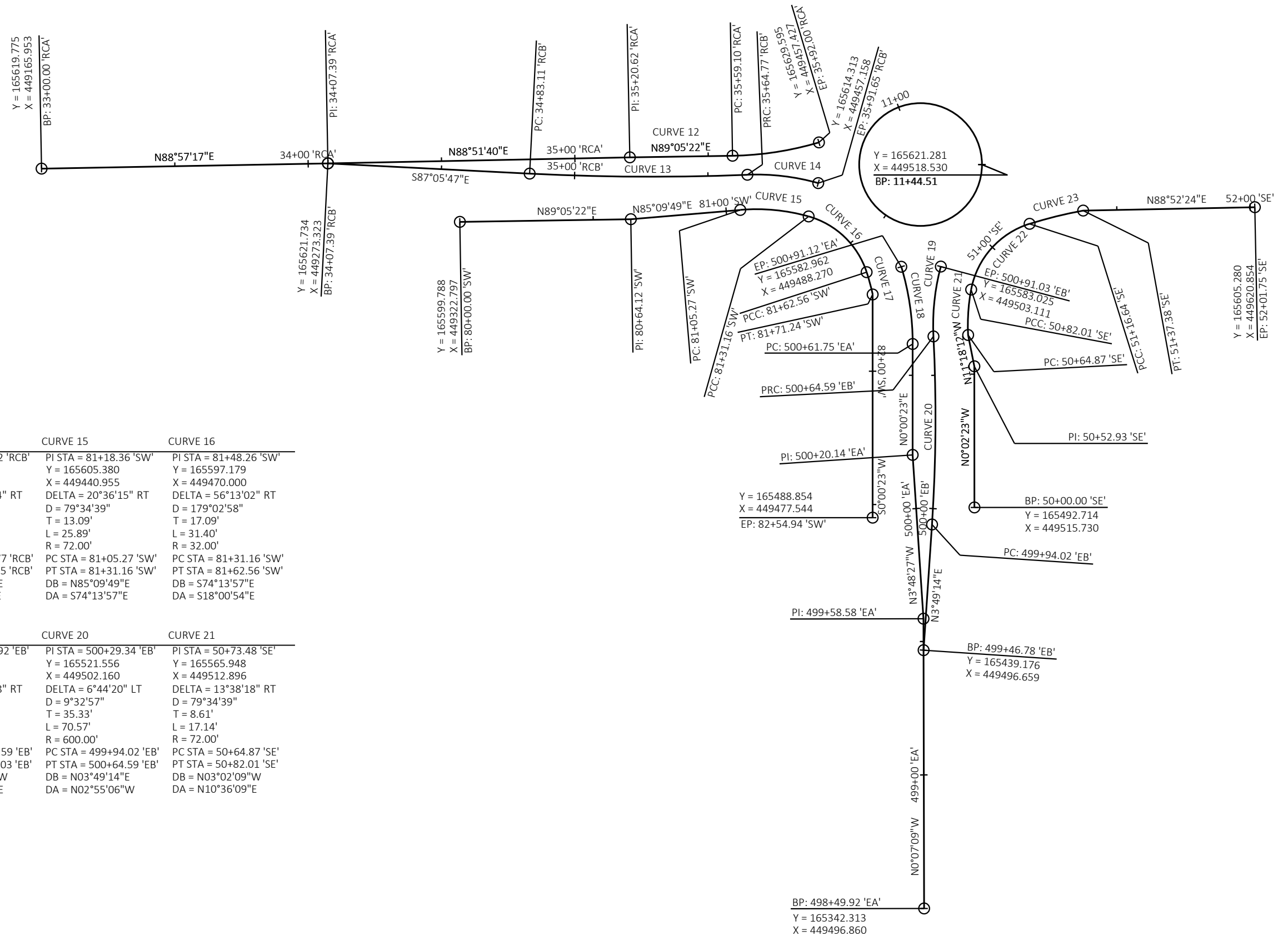


PHASE 5:
-CONSTRUCT NW SIDEWALK AND TRUCK APRON.

-DETOUR PEDESTRIANS AROUND RAB. UTILIZE SDD 15D30
TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATIONS.

-  TEMPORARY CURB RAMP
-  TEMPORARY PEDESTRIAN SURFACE MATTING
-  PEDESTRIAN PATH
-  TRAFFIC CONTROL DRUM
-  TYPE II BARRICADE WITH ATTACHED SIGN
-  SIGN ON PERMANENT SUPPORT

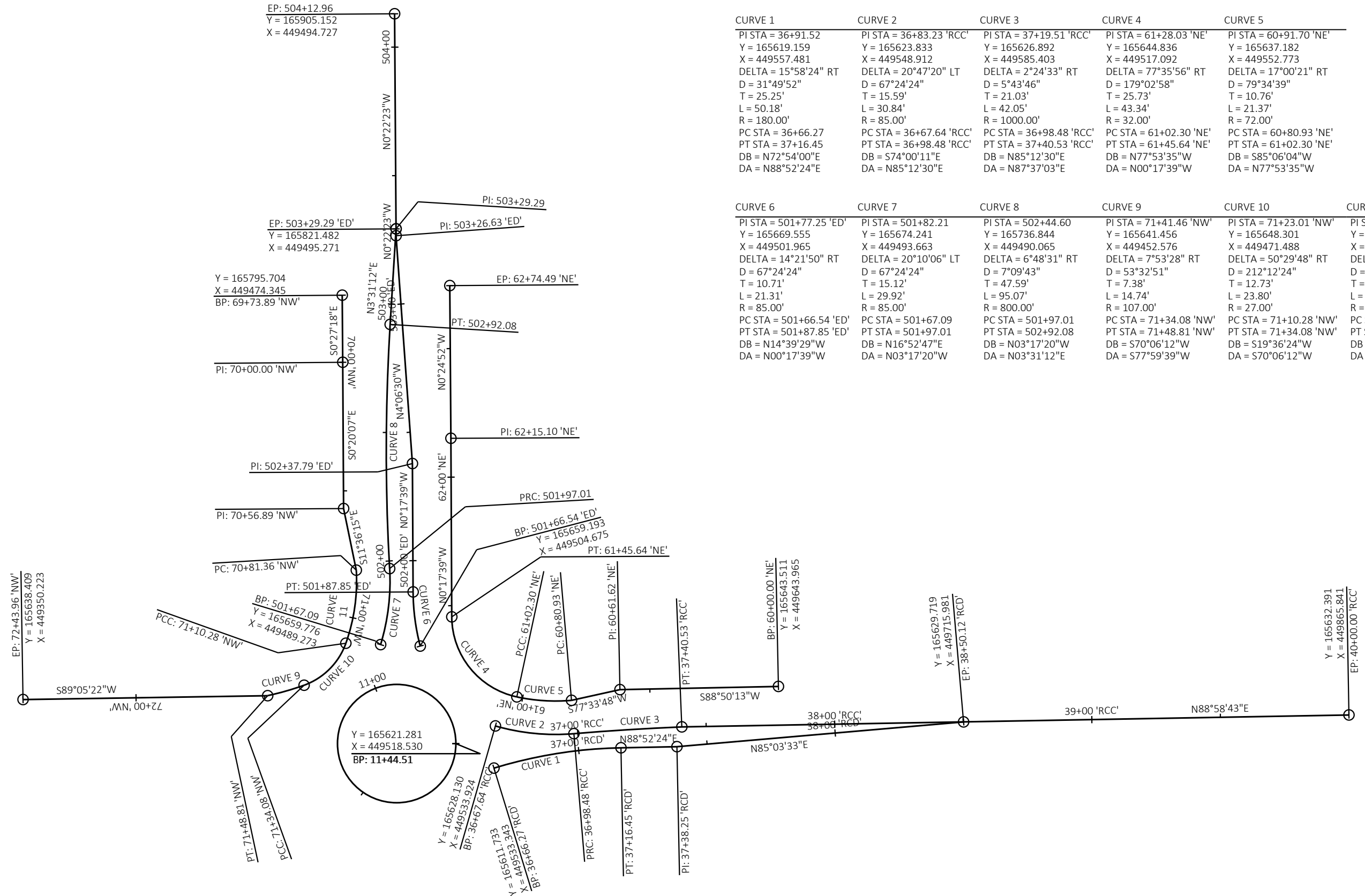




| CURVE 12 | CURVE 13 | CURVE 14 | CURVE 15 | CURVE 16 |
|-------------------------|-------------------------|-------------------------|------------------------|------------------------|
| PI STA = 35+75.66 'RCA' | PI STA = 35+23.97 'RCB' | PI STA = 35+78.32 'RCB' | PI STA = 81+18.36 'SW' | PI STA = 81+48.26 'SW' |
| Y = 165624.859 | Y = 165615.828 | Y = 165618.007 | Y = 165605.380 | Y = 165597.179 |
| X = 449441.562 | X = 449389.756 | X = 449444.121 | X = 449440.955 | X = 449470.000 |
| DELTA = 15°42'41" LT | DELTA = 5°11'55" LT | DELTA = 18°06'54" RT | DELTA = 20°36'15" RT | DELTA = 56°13'02" RT |
| D = 47°44'47" | D = 6°21'58" | D = 67°24'24" | D = 79°34'39" | D = 179°02'58" |
| T = 16.56' | T = 40.86' | T = 13.55' | T = 13.09' | T = 17.09' |
| L = 32.91' | L = 81.66' | L = 26.87' | L = 25.89' | L = 31.40' |
| R = 120.00' | R = 900.00' | R = 85.00' | R = 72.00' | R = 32.00' |
| PC STA = 35+59.10 'RCA' | PC STA = 34+83.11 'RCB' | PC STA = 35+64.77 'RCB' | PC STA = 81+05.27 'SW' | PC STA = 81+31.16 'SW' |
| PT STA = 35+92.00 'RCA' | PT STA = 35+64.77 'RCB' | PT STA = 35+91.65 'RCB' | PT STA = 81+31.16 'SW' | PT STA = 81+62.56 'SW' |
| DB = N89°05'22"E | DB = S87°05'47"E | DB = N87°42'18"E | DB = N85°09'49"E | DB = S74°13'57"E |
| DA = N73°22'41"E | DA = N87°42'18"E | DA = S74°10'48"E | DA = S74°13'57"E | DA = S18°00'54"E |

| CURVE 17 | CURVE 18 | CURVE 19 | CURVE 20 | CURVE 21 |
|------------------------|-------------------------|-------------------------|-------------------------|------------------------|
| PI STA = 81+66.90 'SW' | PI STA = 500+76.54 'EA' | PI STA = 500+77.92 'EB' | PI STA = 500+29.34 'EB' | PI STA = 50+73.48 'SE' |
| Y = 165576.796 | Y = 165568.801 | Y = 165570.146 | Y = 165521.556 | Y = 165565.948 |
| X = 449476.628 | X = 449492.553 | X = 449499.683 | X = 449502.160 | X = 449512.896 |
| DELTA = 5°42'45" RT | DELTA = 16°49'52" LT | DELTA = 17°49'18" RT | DELTA = 6°44'20" LT | DELTA = 13°38'18" RT |
| D = 65°51'26" | D = 57°17'45" | D = 67°24'24" | D = 9°32'57" | D = 79°34'39" |
| T = 4.34' | T = 14.79' | T = 13.33' | T = 35.33' | T = 8.61' |
| L = 8.67' | L = 29.38' | L = 26.44' | L = 70.57' | L = 17.14' |
| R = 87.00' | R = 100.00' | R = 85.00' | R = 600.00' | R = 72.00' |
| PC STA = 81+62.56 'SW' | PC STA = 500+61.75 'EA' | PC STA = 500+64.59 'EB' | PC STA = 499+94.02 'EB' | PC STA = 50+64.87 'SE' |
| PT STA = 81+71.24 'SW' | PT STA = 500+91.12 'EA' | PT STA = 500+91.03 'EB' | PT STA = 500+64.59 'EB' | PT STA = 50+82.01 'SE' |
| DB = S18°00'54"E | DB = N00°00'23"E | DB = N02°55'06"W | DB = N03°49'14"E | DB = N03°02'09"W |
| DA = S12°18'10"E | DA = N16°49'29"W | DA = N14°54'12"E | DA = N02°55'06"W | DA = N10°36'09"E |

| CURVE 22 | CURVE 23 |
|------------------------|------------------------|
| PI STA = 51+01.24 'SE' | PI STA = 51+27.02 'SE' |
| Y = 165593.315 | Y = 165602.163 |
| X = 449518.019 | X = 449546.279 |
| DELTA = 62°00'46" RT | DELTA = 7°06'49" RT |
| D = 179°02'58" | D = 34°18'32" |
| T = 19.23' | T = 10.38' |
| L = 34.63' | L = 20.73' |
| R = 32.00' | R = 167.00' |
| PC STA = 50+82.01 'SE' | PC STA = 51+16.64 'SE' |
| PT STA = 51+16.64 'SE' | PT STA = 51+37.38 'SE' |
| DB = N10°36'09"E | DB = N72°36'55"E |
| DA = N72°36'55"E | DA = N79°43'44"E |



| CURVE 1 | CURVE 2 | CURVE 3 | CURVE 4 | CURVE 5 |
|----------------------|-------------------------|-------------------------|------------------------|------------------------|
| PI STA = 36+91.52 | PI STA = 36+83.23 'RCC' | PI STA = 37+19.51 'RCC' | PI STA = 61+28.03 'NE' | PI STA = 60+91.70 'NE' |
| Y = 165619.159 | Y = 165623.833 | Y = 165626.892 | Y = 165644.836 | Y = 165637.182 |
| X = 449557.481 | X = 449548.912 | X = 449585.403 | X = 449517.092 | X = 449552.773 |
| DELTA = 15°58'24" RT | DELTA = 20°47'20" LT | DELTA = 2°24'33" RT | DELTA = 77°35'56" RT | DELTA = 17°00'21" RT |
| D = 31°49'52" | D = 67°24'24" | D = 5°43'46" | D = 179°02'58" | D = 79°34'39" |
| T = 25.25' | T = 15.59' | T = 21.03' | T = 25.73' | T = 10.76' |
| L = 50.18' | L = 30.84' | L = 42.05' | L = 43.34' | L = 21.37' |
| R = 180.00' | R = 85.00' | R = 1000.00' | R = 32.00' | R = 72.00' |
| PC STA = 36+66.27 | PC STA = 36+67.64 'RCC' | PC STA = 36+98.48 'RCC' | PC STA = 61+02.30 'NE' | PC STA = 60+80.93 'NE' |
| PT STA = 37+16.45 | PT STA = 36+98.48 'RCC' | PT STA = 37+40.53 'RCC' | PT STA = 61+45.64 'NE' | PT STA = 61+02.30 'NE' |
| DB = N72°54'00"E | DB = S74°00'11"E | DB = N85°12'30"E | DB = N77°53'35"W | DB = S85°06'04"W |
| DA = N88°52'24"E | DA = N85°12'30"E | DA = N87°37'03"E | DA = N00°17'39"W | DA = N77°53'35"W |

| CURVE 6 | CURVE 7 | CURVE 8 | CURVE 9 | CURVE 10 | CURVE 11 |
|-------------------------|----------------------|---------------------|------------------------|------------------------|------------------------|
| PI STA = 501+77.25 'ED' | PI STA = 501+82.21 | PI STA = 502+44.60 | PI STA = 71+41.46 'NW' | PI STA = 71+23.01 'NW' | PI STA = 70+96.02 'NW' |
| Y = 165669.555 | Y = 165674.241 | Y = 165736.844 | Y = 165641.456 | Y = 165648.301 | Y = 165674.103 |
| X = 449501.965 | X = 449493.663 | X = 449490.065 | X = 449452.576 | X = 449471.488 | X = 449480.679 |
| DELTA = 14°21'50" RT | DELTA = 20°10'06" LT | DELTA = 6°48'31" RT | DELTA = 7°53'28" RT | DELTA = 50°29'48" RT | DELTA = 23°00'43" RT |
| D = 67°24'24" | D = 67°24'24" | D = 7°09'43" | D = 53°32'51" | D = 212°12'24" | D = 79°34'39" |
| T = 10.71' | T = 15.12' | T = 47.59' | T = 7.38' | T = 12.73' | T = 14.66' |
| L = 21.31' | L = 29.92' | L = 95.07' | L = 14.74' | L = 23.80' | L = 28.92' |
| R = 85.00' | R = 85.00' | R = 800.00' | R = 107.00' | R = 27.00' | R = 72.00' |
| PC STA = 501+66.54 'ED' | PC STA = 501+67.09 | PC STA = 501+97.01 | PC STA = 71+34.08 'NW' | PC STA = 71+10.28 'NW' | PC STA = 70+81.36 'NW' |
| PT STA = 501+87.85 'ED' | PT STA = 501+97.01 | PT STA = 502+92.08 | PT STA = 71+48.81 'NW' | PT STA = 71+34.08 'NW' | PT STA = 71+10.28 'NW' |
| DB = N14°39'29"W | DB = N16°52'47"E | DB = N03°17'20"W | DB = S70°06'12"W | DB = S19°36'24"W | DB = S03°24'19"E |
| DA = N00°17'39"W | DA = N03°17'20"W | DA = N03°31'12"E | DA = S77°59'39"W | DA = S70°06'12"W | DA = S19°36'24"W |

| POINT TABLE | | | | |
|-------------|------------|------------|-----------|-----------------|
| POINT NAME | Y | X | ELEVATION | DESCRIPTION |
| 1 | 165420.033 | 449496.597 | 713.57 | CP 1 |
| 2 | 165656.192 | 449522.048 | 718.48 | CP 2 |
| 1483 | 165758.311 | 449521.291 | 718.79 | BM 2 /SPK IN PP |
| 1484 | 165563.605 | 449521.679 | 717.99 | BM 1 /SPK IN PP |
| 1487 | 165844.541 | 449527.918 | 717.53 | MON /IRON PIPE |
| 1488 | 165932.931 | 449527.722 | 717.29 | MON /IRON PIPE |

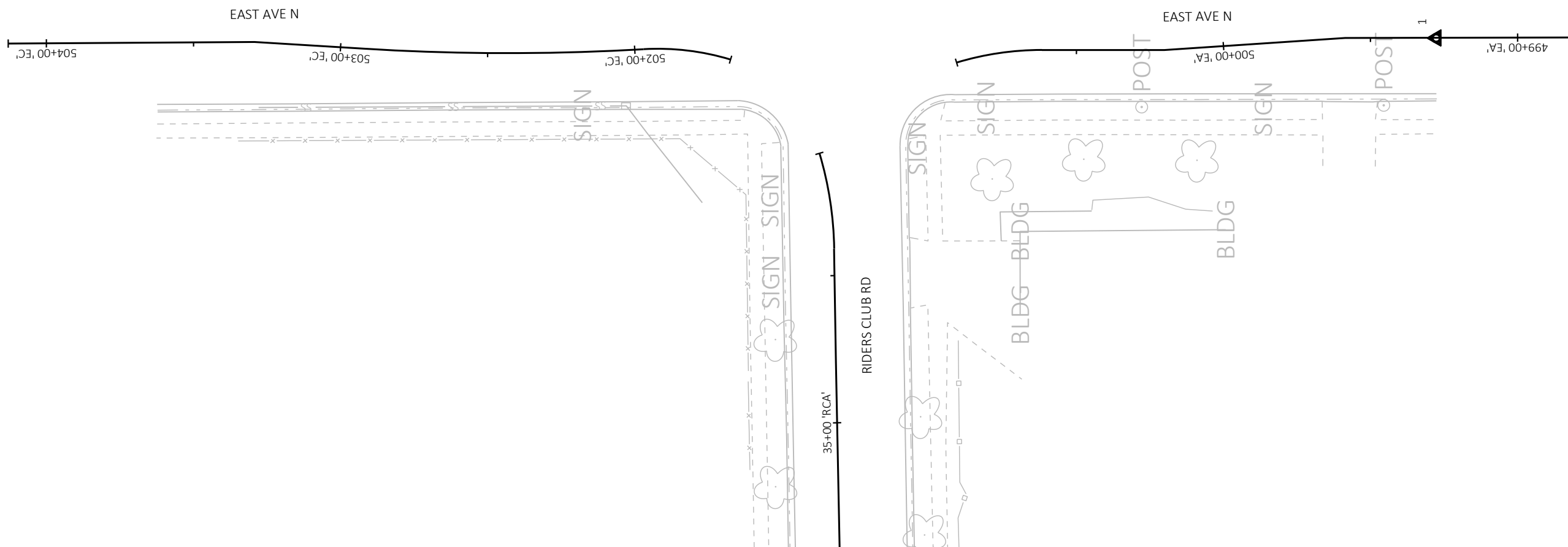


1488

1487

1483

1484



Estimate Of Quantities

5991-02-62

| Line | Item | Item Description | Unit | Total | Qty |
|------|----------|---|------|-----------|-----------|
| 0002 | 204.0100 | Removing Concrete Pavement | SY | 31.000 | 31.000 |
| 0004 | 204.0150 | Removing Curb & Gutter | LF | 482.000 | 482.000 |
| 0006 | 204.0155 | Removing Concrete Sidewalk | SY | 251.000 | 251.000 |
| 0008 | 205.0100 | Excavation Common | CY | 1,067.000 | 1,067.000 |
| 0010 | 213.0100 | Finishing Roadway (project) 01. 5991-02-62 | EACH | 1.000 | 1.000 |
| 0012 | 305.0120 | Base Aggregate Dense 1 1/4-Inch | TON | 1,592.000 | 1,592.000 |
| 0014 | 405.0100 | Coloring Concrete WisDOT Red | CY | 101.000 | 101.000 |
| 0016 | 416.0160 | Concrete Driveway 6-Inch | SY | 26.000 | 26.000 |
| 0018 | 416.0512 | Concrete Truck Apron 12-Inch | SY | 297.000 | 297.000 |
| 0020 | 455.0605 | Tack Coat | GAL | 86.000 | 86.000 |
| 0022 | 460.2000 | Incentive Density HMA Pavement | DOL | 500.000 | 500.000 |
| 0024 | 460.5223 | HMA Pavement 3 LT 58-28 S | TON | 241.000 | 241.000 |
| 0026 | 460.5224 | HMA Pavement 4 LT 58-28 S | TON | 241.000 | 241.000 |
| 0028 | 601.0411 | Concrete Curb & Gutter 30-Inch Type D | LF | 286.000 | 286.000 |
| 0030 | 601.0553 | Concrete Curb & Gutter 4-Inch Sloped 36-Inch Type D | LF | 177.000 | 177.000 |
| 0032 | 602.0405 | Concrete Sidewalk 4-Inch | SF | 2,560.000 | 2,560.000 |
| 0034 | 602.0415 | Concrete Sidewalk 6-Inch | SF | 140.000 | 140.000 |
| 0036 | 602.0515 | Curb Ramp Detectable Warning Field Natural Patina | SF | 80.000 | 80.000 |
| 0038 | 611.0430 | Reconstructing Inlets | EACH | 1.000 | 1.000 |
| 0040 | 611.8110 | Adjusting Manhole Covers | EACH | 1.000 | 1.000 |
| 0042 | 618.0100 | Maintenance And Repair of Haul Roads (project) 01. 5991-02-62 | EACH | 1.000 | 1.000 |
| 0044 | 619.1000 | Mobilization | EACH | 1.000 | 1.000 |
| 0046 | 620.0300 | Concrete Median Sloped Nose | SF | 164.000 | 164.000 |
| 0048 | 624.0100 | Water | MGAL | 35.000 | 35.000 |
| 0050 | 625.0100 | Topsoil | SY | 189.000 | 189.000 |
| 0052 | 628.1905 | Mobilizations Erosion Control | EACH | 3.000 | 3.000 |
| 0054 | 628.1910 | Mobilizations Emergency Erosion Control | EACH | 2.000 | 2.000 |
| 0056 | 628.7010 | Inlet Protection Type B | EACH | 1.000 | 1.000 |
| 0058 | 628.7015 | Inlet Protection Type C | EACH | 11.000 | 11.000 |
| 0060 | 629.0210 | Fertilizer Type B | CWT | 0.440 | 0.440 |
| 0062 | 631.0300 | Sod Water | MGAL | 9.000 | 9.000 |
| 0064 | 631.1000 | Sod Lawn | SY | 312.000 | 312.000 |
| 0066 | 634.0814 | Posts Tubular Steel 2x2-Inch X 14-FT | EACH | 2.000 | 2.000 |
| 0068 | 634.0816 | Posts Tubular Steel 2x2-Inch X 16-FT | EACH | 8.000 | 8.000 |
| 0070 | 637.2210 | Signs Type II Reflective H | SF | 40.520 | 40.520 |
| 0072 | 637.2230 | Signs Type II Reflective F | SF | 34.000 | 34.000 |
| 0074 | 638.2102 | Moving Signs Type II | EACH | 2.000 | 2.000 |
| 0076 | 638.2602 | Removing Signs Type II | EACH | 4.000 | 4.000 |
| 0078 | 638.3000 | Removing Small Sign Supports | EACH | 6.000 | 6.000 |
| 0080 | 642.5001 | Field Office Type B | EACH | 1.000 | 1.000 |
| 0082 | 643.0300 | Traffic Control Drums | DAY | 188.000 | 188.000 |
| 0084 | 643.0410 | Traffic Control Barricades Type II | DAY | 188.000 | 188.000 |
| 0086 | 643.0420 | Traffic Control Barricades Type III | DAY | 940.000 | 940.000 |
| 0088 | 643.0705 | Traffic Control Warning Lights Type A | DAY | 1,316.000 | 1,316.000 |
| 0090 | 643.0900 | Traffic Control Signs | DAY | 2,679.000 | 2,679.000 |
| 0092 | 643.1050 | Traffic Control Signs PCMS | DAY | 28.000 | 28.000 |
| 0094 | 643.3150 | Temporary Marking Line Removable Tape 4-Inch | LF | 129.000 | 129.000 |
| 0096 | 643.5000 | Traffic Control | EACH | 1.000 | 1.000 |
| 0098 | 644.1440 | Temporary Pedestrian Surface Matting | SF | 445.000 | 445.000 |

Estimate Of Quantities

5991-02-62

| Line | Item | Item Description | Unit | Total | Qty |
|------|----------|--|------|-----------|-----------|
| 0100 | 644.1601 | Temporary Pedestrian Curb Ramp | DAY | 160.000 | 160.000 |
| 0102 | 646.8220 | Marking Island Nose Epoxy | EACH | 4.000 | 4.000 |
| 0104 | 646.9010 | Marking Removal Line Water Blasting 4-Inch | LF | 567.000 | 567.000 |
| 0106 | 650.4500 | Construction Staking Subgrade | LF | 541.000 | 541.000 |
| 0108 | 650.5000 | Construction Staking Base | LF | 541.000 | 541.000 |
| 0110 | 650.5500 | Construction Staking Curb Gutter and Curb & Gutter | LF | 1,272.000 | 1,272.000 |
| 0112 | 650.8501 | Construction Staking Electrical Installations (project) 01. 5991-02-62 | EACH | 1.000 | 1.000 |
| 0114 | 650.9000 | Construction Staking Curb Ramps | EACH | 8.000 | 8.000 |
| 0116 | 650.9500 | Construction Staking Sidewalk (project) 01. 5991-02-62 | EACH | 1.000 | 1.000 |
| 0118 | 650.9911 | Construction Staking Supplemental Control (project) 01. 5991-02-62 | EACH | 1.000 | 1.000 |
| 0120 | 652.0225 | Conduit Rigid Nonmetallic Schedule 40 2-Inch | LF | 355.000 | 355.000 |
| 0122 | 653.0154 | Pull Boxes Non-Conductive 24x36-Inch | EACH | 2.000 | 2.000 |
| 0124 | 654.0105 | Concrete Bases Type 5 | EACH | 4.000 | 4.000 |
| 0126 | 655.0610 | Electrical Wire Lighting 12 AWG | LF | 640.000 | 640.000 |
| 0128 | 655.0620 | Electrical Wire Lighting 8 AWG | LF | 1,245.000 | 1,245.000 |
| 0130 | 657.0255 | Transformer Bases Breakaway 11 1/2-Inch Bolt Circle | EACH | 4.000 | 4.000 |
| 0132 | 657.0322 | Poles Type 5-Aluminum | EACH | 4.000 | 4.000 |
| 0134 | 657.0710 | Luminaire Arms Truss Type 4 1/2-Inch Clamp 12-FT | EACH | 4.000 | 4.000 |
| 0136 | 659.1120 | Luminaires Utility LED B | EACH | 4.000 | 4.000 |
| 0138 | 690.0150 | Sawing Asphalt | LF | 157.000 | 157.000 |
| 0140 | 690.0250 | Sawing Concrete | LF | 88.000 | 88.000 |
| 0142 | SPV.0060 | Special 01. Convert Manhole to Valve Box | EACH | 4.000 | 4.000 |
| 0144 | SPV.0060 | Special 02. Salvage and Relocate Hydrant | EACH | 1.000 | 1.000 |
| 0146 | SPV.0060 | Special 03. Gate Valve and Box 6-Inch | EACH | 1.000 | 1.000 |
| 0148 | SPV.0060 | Special 04. Marking Arrow Grooved Epoxy | EACH | 2.000 | 2.000 |
| 0150 | SPV.0060 | Special 05. Marking Symbol Grooved Epoxy | EACH | 2.000 | 2.000 |
| 0152 | SPV.0060 | Special 06. Concrete Control Cabinet Base Type Special | EACH | 1.000 | 1.000 |
| 0154 | SPV.0060 | Special 07. Lighting Control Cabinet Type Special | EACH | 1.000 | 1.000 |
| 0156 | SPV.0060 | Special 08. Temporary Lighting | EACH | 1.000 | 1.000 |
| 0158 | SPV.0060 | Special 09. Manhole Covers Special | EACH | 1.000 | 1.000 |
| 0160 | SPV.0085 | Special 01. Water Main Fittings | LB | 49.000 | 49.000 |
| 0162 | SPV.0090 | Special 01. Marking Line Grooved Epoxy 4-Inch | LF | 2,659.000 | 2,659.000 |
| 0164 | SPV.0090 | Special 02. Marking Line Grooved Epoxy 8-Inch | LF | 154.000 | 154.000 |
| 0166 | SPV.0090 | Special 03. Marking Dotted Extension Grooved Epoxy 18-Inch | LF | 41.000 | 41.000 |
| 0168 | SPV.0090 | Special 04. Marking Diagonal Grooved Epoxy 12-Inch | LF | 30.000 | 30.000 |
| 0170 | SPV.0090 | Special 05. Marking Crosswalk Grooved Epoxy Transverse Line 6-Inch | LF | 222.000 | 222.000 |
| 0172 | SPV.0090 | Special 06. Water Main Ductile Iron 6-Inch | LF | 37.000 | 37.000 |
| 0174 | SPV.0090 | Special 07. Concrete Curb & Gutter 4-Inch Sloped 24-Inch Type D | LF | 683.000 | 683.000 |
| 0176 | SPV.0090 | Special 08. Concrete Curb & Gutter 3-Inch Sloped 36-Inch Type T | LF | 126.000 | 126.000 |
| 0178 | SPV.0165 | Special 01. Vegetated Porous Pavement | SF | 1,003.000 | 1,003.000 |

3

REMOVING CONCRETE ITEMS

| CATEGORY | STATION | SY | LF | SY | COMMENT |
|-----------------------|---------------------------|-----------|------------|------------|----------------|
| 0010 | 70+56 CR-NW - 71+70 CR-NW | - | 112 | 54 | NW QUAD |
| | 80+54 CR-SW - 82+12 CR-SW | - | 160 | 57 | SW QUAD |
| | 60+61 CR-NE - 61+73 CR-NE | - | 112 | 62 | NE QUAD |
| | 50+52 CR-SE - 51+48 CR-SE | - | 98 | 78 | SE QUAD |
| | STA 35+50 RCA | 31 | - | - | DRIVEWAY APRON |
| PROJECT TOTALS | | 31 | 482 | 251 | |

BASE AGGREGATE DENSE

| CATEGORY | LOCATION | TON | MGAL | REMARKS |
|----------------------|---------------|-------------|-----------|--------------------|
| 0010 | MAINLINE | 1442 | 29 | 12" UNDER MAINLINE |
| | WEST ISLAND | 20 | 1 | 6" UNDER ISLAND |
| | EAST ISLAND | 16 | 1 | 6" UNDER ISLAND |
| | NORTH ISLAND | 26 | 1 | 6" UNDER ISLAND |
| | SOUTH ISLAND | 18 | 1 | 6" UNDER ISLAND |
| | CENTER ISLAND | 70 | 2 | 6" UNDER ISLAND |
| PROJECT TOTAL | | 1592 | 35 | |

ASPHALT PAVEMENT

| CATEGORY | LOCATION | IN | IN | GAL | TON | TON | REMARKS |
|-----------------------|----------|------|------|-----------|------------|------------|----------|
| 0010 | MAINLINE | 2.50 | 2.50 | 86 | 241 | 241 | MAINLINE |
| PROJECT TOTALS | | | | 86 | 241 | 241 | |

CONCRETE ITEMS

| CATEGORY | STATION - LOCATION | CY | SY | SY | LF | LF | LF | LF | SF | SF | SF | REMARKS |
|-----------------------|---------------------------------|------------|------------|-----------|------------|------------|------------|------------|-------------|------------|------------|----------------|
| 0010 | STA 70+56 NW - STA 71+14 NW, RT | - | - | - | 52 | - | - | - | - | - | - | NW QUADRANT |
| | STA 71+14 NW - STA 71+52 NW, RT | - | - | - | - | 38 | - | - | - | - | - | NW QUADRANT |
| | STA 71+52 NW - STA 71+70 NW, RT | - | - | - | 19 | - | - | - | - | - | - | NW QUADRANT |
| | STA 80+54 SW - STA 81+28 SW, RT | - | - | - | 74 | - | - | - | - | - | - | SW QUADRANT |
| | STA 81+28 SW - STA 81+84 SW, RT | - | - | - | - | 53 | - | - | - | - | - | SW QUADRANT |
| | STA 81+84 SW - STA 82+12 SW, RT | - | - | - | 28 | - | - | - | - | - | - | SW QUADRANT |
| | STA 60+61 NE - STA 61+06 NE, RT | - | - | - | 44 | - | - | - | - | - | - | NE QUADRANT |
| | STA 61+06 NE - STA 61+52 NE, RT | - | - | - | - | 43 | - | - | - | - | - | NE QUADRANT |
| | STA 61+52 NE - STA 61+73 NE, RT | - | - | - | 21 | - | - | - | - | - | - | NE QUADRANT |
| | STA 50+52 SE - STA 50+85 SE, RT | - | - | - | 32 | - | - | - | - | - | - | SE QUADRANT |
| | STA 50+85 SE - STA 51+31 SE, RT | - | - | - | - | 43 | - | - | - | - | - | SE QUADRANT |
| | STA 51+31 SE - STA 51+47 SE, RT | - | - | - | 16 | - | - | - | - | - | - | SE QUADRANT |
| | NORTH ISLAND | 18 | 52 | - | - | - | 188 | - | - | - | - | NORTH ISLAND |
| | STA 502+58 EC | - | - | - | - | - | - | - | - | - | 13 | TYPE 1 |
| | STA 501+70 EC | - | - | - | - | - | - | - | - | - | 4 | TYPE 2 |
| | STA 501+73 EC | - | - | - | - | - | - | - | - | - | 13 | TYPE 2 |
| | SOUTH ISLAND | 12 | 35 | - | - | - | 143 | - | - | - | - | SOUTH ISLAND |
| | STA 500+23 EA | - | - | - | - | - | - | - | - | - | 29 | TYPE 1 |
| | STA 500+86 EA | - | - | - | - | - | - | - | - | - | 4 | TYPE 2 |
| | STA 500+87 EA | - | - | - | - | - | - | - | - | - | 13 | TYPE 2 |
| | WEST ISLAND | 13 | 39 | - | - | - | 202 | - | - | - | - | WEST ISLAND |
| | STA 34+93 RCA | - | - | - | - | - | - | - | - | - | 14 | TYPE 1 |
| | STA 35+86 RCA | - | - | - | - | - | - | - | - | - | 4 | TYPE 2 |
| | STA 35+88 RCA | - | - | - | - | - | - | - | - | - | 14 | TYPE 2 |
| | EAST ISLAND | 11 | 31 | - | - | - | 150 | - | - | - | - | EAST ISLAND |
| | STA 37+38 RCC | - | - | - | - | - | - | - | - | - | 38 | TYPE 1 |
| | STA 36+70 RCC | - | - | - | - | - | - | - | - | - | 4 | TYPE 2 |
| | STA 36+73 RCC | - | - | - | - | - | - | - | - | - | 14 | TYPE 2 |
| | CENTER ISLAND | 47 | 140 | - | - | - | - | 126 | - | - | - | CENTER ISLAND |
| | NW QUADRANT | - | - | - | - | - | - | - | 597 | - | - | NW QUADRANT |
| | SW QUADRANT | - | - | - | - | - | - | - | 583 | - | - | SW QUADRANT |
| | NE QUADRANT | - | - | - | - | - | - | - | 644 | - | - | NE QUADRANT |
| | SE QUADRANT | - | - | - | - | - | - | - | 736 | - | - | SE QUADRANT |
| | STA 35+50 RCA, RT | - | - | - | - | - | - | - | - | 140 | - | SW AT DRIVEWAY |
| | STA 35+50 RCA, RT | - | - | 26 | - | - | - | - | - | - | - | DRIVEWAY APRON |
| PROJECT TOTALS | | 101 | 297 | 26 | 286 | 177 | 683 | 126 | 2560 | 140 | 164 | |

3

PROJECT NO: 5991-02-62

HWY: EAST AVENUE N

COUNTY: LA CROSSE

MISCELLANEOUS QUANTITIES

SHEET

E

EARTHWORK SUMMARY

| DIVISION | FROM/TO STATION | LOCATION | 205.0100 COMMON EXCAVATION (1) | AVAILABLE MATERIAL (5) | UNEXPANDED FILL | EXPANDED FILL (13) | MASS ORDINATE +/- (14) | WASTE |
|--|-----------------------|----------|---|------------------------------|--------------------|-----------------------|---------------------------|-------|
| | | | CUT (2) | | | FACTOR 1.30 | | |
| DIVISION 1 | | | | | | | | |
| CENTER ISLAND - CID ALIGNMENT | 10+00/11+25 | | 212 | 212 | 0 | 0 | 212 | 212 |
| 1-EAST AVE NORTHBOUND ENTRY - EB ALIGNMENT | 500+14.007/500+88.045 | | 72 | 72 | 10 | 13 | 59 | 59 |
| 2-EAST AVE NORTHBOUND EXIT - ED ALIGNMENT | 501+72.176/502+89.747 | | 134 | 134 | 0 | 0 | 134 | 134 |
| 3-EAST AVE SOUTHBOUND ENTRY - EC ALIGNMENT | 501+67.091/502+90 | | 139 | 139 | 4 | 5 | 134 | 134 |
| 4-EAST AVE SOUTHBOUND EXIT - EEA ALIGNMENT | 500+14/500+91.124 | | 90 | 90 | 0 | 0 | 90 | 90 |
| 5-RIDERS CLUB RD EASTBOUND ENTRY - RCB ALIGNMENT | 34+85.174/35+91.649 | | 126 | 126 | 2 | 3 | 123 | 123 |
| 6-RIDERS CLUB RD EASTBOUND EXIT - RCD ALIGNMENT | 36+72.564/37+49.769 | | 85 | 85 | 1 | 1 | 84 | 84 |
| 7-RIDERS CLUB RD WESTBOUND ENTRY - RCC ALIGNMENT | 36+67.641/37+50 | | 91 | 91 | 3 | 4 | 87 | 87 |
| 8-RIDERS CLUB RD WESTBOUND EXIT - RCA ALIGNMENT | 34+85/35+92.005 | | 118 | 118 | 0 | 0 | 118 | 118 |
| DIVISION 1 SUBTOTAL | | | 1,067 | 1,067 | 20 | 26 | 1,041 | 1,041 |
| GRAND TOTAL | | | 1,067 | 1,067 | 20 | 26 | 1,041 | 1,041 |
| TOTAL COMMON EXC | | | 1,067 | | | | | |

NOTES:

(1) COMMON EXCAVATION IS THE SUM OF THE CUT AND EBS EXCAVATION COLUMNS. ITEM NUMBER 205.0100

(2) SALVAGED/UNUSABLE PAVEMENT MATERIAL IS INCLUDED IN CUT.

5) AVAILABLE MATERIAL = CUT - SALVAGED/UNUSABLE PAVEMENT MATERIAL

(13) EXPANDED FILL FACTOR = 1.30

DEPENDENT ON SELECTIONS:

EXPANDED FILL = (UNEXPANDED FILL) * FILL FACTOR

(14) THE MASS ORDINATE + OR - QTY CALCULATED FOR THE DIVISION. PLUS QUANTITY INDICATES AN EXCESS OF MATERIAL WITHIN THE DIVISION. MINUS INDICATES A SHORTAGE OF MATERIAL WITHIN THE DIVISION.

(15) FACTORS USED TO COMPUTE ANTICIPATED WASTE AND THE COMPUTED WASTE VOLUME IDENTIFIED ARE FOR GENERAL INFORMATION ONLY.

3

DETECTABLE WARNING FIELD

| CATEGORY | LOCATION | SF |
|----------------|-----------|----|
| 0010 | NORTH LEG | 20 |
| | SOUTH LEG | 20 |
| | WEST LEG | 20 |
| | EAST LEG | 20 |
| PROJECT TOTALS | | 80 |

EROSION CONTROL

| CATEGORY | LOCATION | 628.1910 MOBILIZATIONS EROSION CONTROL EACH | 628.1905 MOBILIZATIONS EROSION CONTROL EACH | 628.7010 INLET PROTECTION TYPE B EACH | 628.7015 INLET PROTECTION TYPE C EACH | REMARKS |
|----------------|-----------|---|---|---|---|--------------------------------|
| 0010 | PROJECT | 3 | 2 | - | - | |
| | NORTH LEG | - | - | 1 | 5 | NORTH LEG OUTSIDE PROJECT AREA |
| | SOUTH LEG | - | - | - | 4 | SOUTH LEG OUTSIDE PROJECT AREA |
| | WEST LEG | - | - | - | 2 | WEST LEG OUTSIDE PROJECT AREA |
| PROJECT TOTALS | | 3 | 2 | 1 | 11 | |

LANDSCAPING

| CATEGORY | LOCATION | 625.0100 TOPSOIL SY | 629.0210 FERTILIZER CWT | 631.0300 SOD WATER MGAL | 631.1000 SOD LAWN SY |
|----------------|-------------------|---------------------------|-------------------------------|----------------------------------|-------------------------------|
| 0010 | NW QUADRANT | 21 | 0.10 | 2.00 | 48 |
| | SW QUADRANT | 43 | 0.10 | 2.00 | 74 |
| | NE QUADRANT | 49 | 0.10 | 2.00 | 73 |
| | SE QUADRANT | 59 | 0.10 | 2.00 | 89 |
| | UNDISTRIBUTED QTY | 17.2 | 0.04 | 0.80 | 28.4 |
| PROJECT TOTALS | | 189 | 0.44 | 9 | 312 |

PERMANENT SIGNING

| CATEGORY | STATION | SIGN CODE | SIGN MESSAGE | SIZE W" X H" | 637.2210 SIGN TYPE II REFLECTIVE H SF | 637.2230 SIGN TYPE II REFLECTIVE F SF | 638.2102 MOVING SIGN TYPE II EACH | 638.2602 REMOVING SIGN TYPE II EACH | 638.3000 REMOVING SMALL SIGN SUPPORTS EACH | 634.0814 POST TUBULAR STEEL 14-FT EACH | 634.0816 POST TUBULAR STEEL 16-FT EACH | COMMENT |
|----------------|---------------|--------------|-------------------|-----------------|---|---|---|---|---|---|---|-----------|
| 0010 | 34+27 RCA, RT | W2-6 | ROUNDAABOUT AHEAD | 30" X 30" | - | 6.25 | - | - | - | - | 1 | WEST LEG |
| | 34+27 RCA, RT | W13-1 | 15 MPH | 18" X 18" | - | 2.25 | - | - | - | - | - | WEST LEG |
| | 35+77 RCA, RT | R1-2 | YIELD | 36" X 31" | 3.88 | - | - | - | - | - | 1 | WEST LEG |
| | 35+77 RCA, RT | R6-5P | TRAFFIC CIRCLE | 30" X 30" | 6.25 | - | - | - | - | - | - | WEST LEG |
| | 38+22 RCC, LT | W2-6 | ROUNDAABOUT AHEAD | 30" X 30" | - | 6.25 | - | - | - | - | 1 | EAST LEG |
| | 38+22 RCC, LT | W13-1 | 15 MPH | 18" X 18" | - | 2.25 | - | - | - | - | - | EAST LEG |
| | 36+72 RCC, LT | R1-2 | YIELD | 36" X 31" | 3.88 | - | - | - | - | - | 1 | EAST LEG |
| | 36+72 RCC, LT | R6-5P | TRAFFIC CIRCLE | 30" X 30" | 6.25 | - | - | - | - | - | - | EAST LEG |
| | 503+25 EC, LT | W2-6 | ROUNDAABOUT AHEAD | 30" X 30" | - | 6.25 | - | - | - | - | 1 | NORTH LEG |
| | 503+25 EC, LT | W13-1 | 15 MPH | 18" X 18" | - | 2.25 | - | - | - | - | - | NORTH LEG |
| | 501+75 EC, LT | R1-2 | YIELD | 36" X 31" | 3.88 | - | - | - | - | - | 1 | NORTH LEG |
| | 501+75 EC, LT | R6-5P | TRAFFIC CIRCLE | 30" X 30" | 6.25 | - | - | - | - | - | - | NORTH LEG |
| | 499+25 EA, RT | W2-6 | ROUNDAABOUT AHEAD | 30" X 30" | - | 6.25 | - | - | - | - | 1 | SOUTH LEG |
| | 499+25 EA, RT | W13-1 | 15 MPH | 18" X 18" | - | 2.25 | - | - | - | - | - | SOUTH LEG |
| | 500+75 EA, RT | R1-2 | YIELD | 36" X 31" | 3.88 | - | - | - | - | - | 1 | SOUTH LEG |
| | 500+75 EA, RT | R6-5P | TRAFFIC CIRCLE | 30" X 30" | 6.25 | - | - | - | - | - | - | SOUTH LEG |
| | 35+75 RCA, LT | - | NO TRUCKS | - | - | - | 1 | - | - | 1 | - | |
| | 500+90 EA, LT | - | NO TRUCKS | - | - | - | 1 | - | - | 1 | - | |
| | SW QUAD | - | STOP SIGN | - | - | - | - | 1 | 2 | - | - | |
| | NE QUAD | - | STOP SIGN | - | - | - | - | 1 | 2 | - | - | |
| | RIDGEWAY AVE | - | STOP AHEAD | - | - | - | - | 1 | 1 | - | - | |
| | JOHNSON ST | - | STOP AHEAD | - | - | - | - | 1 | 1 | - | - | |
| PROJECT TOTALS | | | | | 40.52 | 34 | 2 | 4 | 6 | 2 | 8 | |

3

3

3

TRAFFIC CONTROL

| CATEGORY | LOCATION | APPROX. SERVICE PERIOD | 643.0300 TRAFFIC CONTROL | | 643.0410 TRAFFIC CONTROL | | 643.0420 TRAFFIC CONTROL | | | 643.0705 TRAFFIC CONTROL | | 643.0900 TRAFFIC CONTROL | | 643.1050 TRAFFIC CONTROL | | NOTES |
|----------------|--------------------|------------------------|--------------------------|------|--------------------------|---------------------|--------------------------|-------|------------|--------------------------|------|--------------------------|------|--------------------------|------|---------------------------------|
| | | | DRUMS | QTY. | BARRICADES TYPE II | BARRICADES TYPE III | WARNING LIGHTS TYPE A | SIGNS | SIGNS PCMS | QTY. | DAYS | QTY. | DAYS | QTY. | DAYS | |
| 0010 | PRE - CONSTRUCTION | 7 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | 28 | PRE-WARN, 7 DAYS PRIOR TO START |
| | PROJECT AREA | 47 | 0 | 0 | 0 | 0 | 20 | 940 | 28 | 1316 | 33 | 1551 | 0 | 0 | | ENTIRE DURATION OF PROJECT |
| | PEDESTRIAN STAGING | 47 | 4 | 188 | 4 | 188 | 0 | 0 | 0 | 0 | 24 | 1128 | 0 | 0 | | ENTIRE DURATION OF PROJECT |
| PROJECT TOTALS | | | | 188 | | 188 | | 940 | | 1316 | | 2679 | | 28 | | |

TEMPORARY PEDESTRIAN

| CATEGORY | STATION | APPROX. SERVICE PERIOD | 644.1440 TEMPORARY PEDESTRIAN | | 643.3150 TEMPORARY MARKING LINE | | COMMENT |
|----------------|------------------|------------------------|-------------------------------|-----|---------------------------------|-----|---------|
| | | | SURFACE MATTING | QTY | PEDESTRIAN CURB RAMP | DAY | |
| 0010 | TEMP. NORTH RAMP | 40 | 42 | 2 | 80 | 68 | |
| | TEMP. SOUTH RAMP | 40 | 63 | 2 | 80 | 61 | |
| | NW QUADRANT | 40 | 85 | 0 | 0 | - | |
| | SW QUADRANT | 40 | 85 | 0 | 0 | - | |
| | NE QUADRANT | 40 | 85 | 0 | 0 | - | |
| | SE QUADRANT | 40 | 85 | 0 | 0 | - | |
| PROJECT TOTALS | | | 445 | 160 | 129 | | |

PAVEMENT MARKING

| CATEGORY | STATION - STATION | LOCATION | SPV.0090.01 | | | | | | | | | | REMARKS | | |
|----------------------|------------------------------|---------------|----------------------|--------------|-------|---------------|---------------|----------|---------------|-------------------|------------|------------------|-------------------|-------------------|------|
| | | | MARKING LINE | | | MARKING LINE | | MARKING | MARKING | MARKING DOTTED | | MARKING DIAGONAL | | MARKING CROSSWALK | |
| | | | GROOVED EPOXY 4-INCH | | | GROOVED EPOXY | | ARROW | SYMBOL | EXTENSION GROOVED | | GROOVED EPOXY | | GROOVED EPOXY | |
| | | | SOLID | SOLID | SKIPS | 8-INCH | GROOVED EPOXY | ARROW | GROOVED EPOXY | EPOXY 18-INCH | 12-INCH | TRANSVERSE | | ISLAND | NOSE |
| WHITE | YELLOW | WHITE | WHITE | WHITE | WHITE | WHITE | WHITE | YELLOW | WHITE | EPOXY | EACH | | | | |
| LF | LF | LF | LF | EACH | EACH | LF | LF | LF | LF | EACH | | | | | |
| 0010 | 501+75 EC - 503+28 EC, LT/RT | NORTH LEG | - | 425 | - | - | - | - | - | - | - | - | ISLAND MARKINGS | | |
| | 501+67 EC - 501+79 EC, LT/RT | NORTH LEG | - | - | - | 39 | - | - | - | - | - | - | ISLAND MARKINGS | | |
| | 501+89 EC - 501+95 EC, LT/RT | NORTH LEG | - | - | - | - | - | - | - | - | 56 | - | CROSSWALK | | |
| | 501+67 EC, LT | NORTH LEG | - | - | - | - | - | - | 11 | - | - | - | | | |
| | STA 502+58 EC | NORTH LEG | - | - | - | - | - | - | - | - | - | 1 | ISLAND NOSE | | |
| | 499+46 EA - 500+80 EA, LT/RT | SOUTH LEG | - | 397 | - | - | - | - | - | - | - | - | ISLAND MARKINGS | | |
| | 499+54 EA - 500+19 EA, RT | SOUTH LEG | - | - | - | - | - | - | - | 13 | - | - | ISLAND MARKINGS | | |
| | 500+80 EA - 500+91 EA, LT/RT | SOUTH LEG | - | - | - | 37 | - | - | - | - | - | - | ISLAND MARKINGS | | |
| | 500+58 EA - 500+64 EA, LT/RT | SOUTH LEG | - | - | - | - | - | - | - | - | 58 | - | CROSSWALK | | |
| | 500+87 EA, RT | SOUTH LEG | - | - | - | - | - | - | 11 | - | - | - | | | |
| | STA 500+23 EA | SOUTH LEG | - | - | - | - | - | - | - | - | - | 1 | ISLAND NOSE | | |
| | 33+99 RCA - 35+08 RCA, RT | WEST LEG | 213 | - | - | - | - | - | - | - | - | - | BIKE LANE MARKING | | |
| | 34+85 RCA - 35+32 RCA, LT | WEST LEG | 48 | - | - | - | - | - | - | - | - | - | BIKE LANE MARKING | | |
| | 33+99 RCA - 35+81 RCA, LT/RT | WEST LEG | - | 562 | - | - | - | - | - | - | - | - | ISLAND MARKINGS | | |
| | 35+04 RCA - 35+59 RCA, RT | WEST LEG | - | - | 14 | - | - | - | - | - | - | - | BIKE LANE MARKING | | |
| | 35+32 RCA - 35+69 RCA, LT | WEST LEG | - | - | 10 | - | - | - | - | - | - | - | BIKE LANE MARKING | | |
| | 35+81 RCA - 35+92 RCA, LT/RT | WEST LEG | - | - | - | 39 | - | - | - | - | - | - | ISLAND MARKINGS | | |
| | 35+02 RCA, LT | WEST LEG | - | - | - | - | 1 | - | - | - | - | - | BIKE LANE MARKING | | |
| | 35+10 RCA, LT | WEST LEG | - | - | - | - | - | 1 | - | - | - | - | BIKE LANE MARKING | | |
| | 35+68 RCA - 35+74, LT/RT | WEST LEG | - | - | - | - | - | - | - | - | 55 | - | CROSSWALK | | |
| | 35+88 RCA, RT | WEST LEG | - | - | - | - | - | - | 8 | - | - | - | | | |
| | STA 34+93 RCA | WEST LEG | - | - | - | - | - | - | - | - | - | 1 | ISLAND NOSE | | |
| | 36+67 RCC, LT | EAST LEG | - | - | - | - | - | - | 11 | - | - | - | | | |
| | 36+67 RCC - 36+80 RCC, LT/RT | EAST LEG | - | - | - | 39 | - | - | - | - | - | - | ISLAND MARKINGS | | |
| | 36+85 RCC - 36+91 RCC, LT/RT | EAST LEG | - | - | - | - | - | - | - | - | 53 | - | CROSSWALK | | |
| | 36+76 RCC - 38+50 RCC, LT/RT | EAST LEG | - | 544 | - | - | - | - | - | - | - | - | ISLAND MARKINGS | | |
| | 37+44 RCC - 38+32 RCC, RT | EAST LEG | - | - | - | - | - | - | - | 17 | - | - | ISLAND MARKINGS | | |
| | 36+91 RCC - 37+33 RCC, RT | EAST LEG | - | - | 11 | - | - | - | - | - | - | - | BIKE LANE MARKING | | |
| | 36+98 RCC - 37+50 RCC, LT | EAST LEG | 53 | - | - | - | - | - | - | - | - | - | BIKE LANE MARKING | | |
| | 37+33 RCC - 38+58 RCC, RT | EAST LEG | 231 | - | - | - | - | - | - | - | - | - | BIKE LANE MARKING | | |
| | 37+50 RCC, RT | EAST LEG | - | - | - | - | 1 | - | - | - | - | - | BIKE LANE MARKING | | |
| | 37+43 RCC, RT | EAST LEG | - | - | - | - | - | 1 | - | - | - | - | BIKE LANE MARKING | | |
| | STA 37+38 RCC | EAST LEG | - | - | - | - | - | - | - | - | - | 1 | ISLAND NOSE | | |
| | CENTER ISLAND FLAG | CENTER ISLAND | - | 151 | - | - | - | - | - | - | - | - | ISLAND MARKINGS | | |
| PROJECT TOTAL | | | | 2,659 | | 154 | 2 | 2 | 41 | 30 | 222 | 4 | | | |

3

MARKING REMOVALS

| 646.9010 MARKING REMOVAL LINE WATER BLASTING 4-INCH | | | |
|--|-----------------------|------------|-----------|
| CATEGORY | LOCATION | LF | REMARKS |
| 0010 | 502+90 EC - 503+29 EC | 40 | NORTH LEG |
| | 499+46 EA - 500+19 EA | 69 | SOUTH LEG |
| | 34+00 RCA - 34+85 RCA | 191 | WEST LEG |
| | 37+50 RCC - 38+58 RCC | 267 | EAST LEG |
| PROJECT TOTAL | | 567 | |

SAWCUT

| 690.0150 690.0250 SAWING SAWING ASPHALT CONCRETE | | | | |
|--|---------------|------------|-----------|----------|
| CATEGORY | STATION | LF | LF | REMARKS |
| 0010 | 502+04 EC, RT | - | 5 | SIDEWALK |
| | 502+07 EC, LT | - | 5 | SIDEWALK |
| | 502+05 EC, RT | - | 2.5 | C&G |
| | 502+21 EC, LT | - | 2.5 | C&G |
| | 502+90 EC | 42 | - | PAVEMENT |
| | 500+14 EA | 38 | - | PAVEMENT |
| | 500+18 EA, RT | - | 5 | SIDEWALK |
| | 500+48 EA, LT | - | 5 | SIDEWALK |
| | 500+39 EA, LT | - | 2.5 | C&G |
| | 500+53 EA, RT | - | 2.5 | C&G |
| | 34+85 RCA | 39 | - | PAVEMENT |
| | 35+10 RCA, RT | - | 2.5 | C&G |
| | 35+58 RCA, LT | - | 2.5 | C&G |
| | 35+33 RCA, RT | - | 5 | SIDEWALK |
| | 35+58 RCA, LT | - | 5 | SIDEWALK |
| | 35+50 RCA, RT | - | 28 | DRIVEWAY |
| | 36+99 RCC, RT | - | 2.5 | C&G |
| | 37+17 RCC, LT | - | 2.5 | C&G |
| | 36+98 RCC, RT | - | 5 | SIDEWALK |
| | 37+18 RCC, LT | - | 5 | SIDEWALK |
| | 37+50 RCC | 38 | - | PAVEMENT |
| PROJECT TOTALS | | 157 | 88 | |

CONSTRUCTION STAKING

| 650.5500 650.8501 650.9911 CONSTRUCTION CONSTRUCTION CONSTRUCTION CONSTRUCTION 650.4500 650.5000 STAKING STAKING 650.9000 STAKING CONSTRUCTION CONSTRUCTION CONSTRUCTION CURB GUTTER ELECTRICAL CONSTRUCTION SUPPLEMENTAL STAKING CONSTRUCTION STAKING STAKING AND CURB INSTALLATIONS STAKING CONTROL STAKING SUBGRADE BASE & GUTTER (PROJECT) CURB RAMPS (PROJECT) (PROJECT) (PROJECT) | | | | | | | | |
|--|---------|------------|------------|-------------|----------|----------|----------|----------|
| CATEGORY | STATION | LF | LF | LF | EACH | EACH | EACH | EACH |
| 0010 | PROJECT | 541 | 541 | 1272 | 1 | 8 | 1 | 1 |
| PROJECT TOTALS | | 541 | 541 | 1272 | 1 | 8 | 1 | 1 |

ADJUSTING UTILITIES

| 611.8110 SPV.0060.01 SPV.0060.02 SPV.0060.03 SPV.0085.01 SPV.0090.06 SPV.0060.09 611.0430 ADJUSTING CONVERT SALVAGE AND GATE VALVE WATER WATER MAIN MANHOLE RECONSTRUCTING MANHOLE MANHOLE TC RELOCATE AND BOX MAIN DUCTILE COVERS INLETS COVERS VALVE BOX HYDRANT 6-INCH FITTINGS IRON 6-INCH SPECIAL | | | | | | | | | | |
|---|-------------------|----------|----------|----------|----------|----------|-----------|-----------|----------|------------------------|
| CATEGORY | STATION | EACH | EACH | EACH | EACH | EACH | LB | LF | EACH | COMMENT |
| 0010 | 502+04 EC, 19' RT | 1 | - | - | - | - | - | - | 1 | |
| CAT 0010 SUBTOTALS | | 1 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | |
| 0020 | 10+27 CID, LT | - | - | 1 | - | - | - | - | - | |
| | 10+81 CID, LT | - | - | 1 | - | - | - | - | - | |
| | 11+36 CID, LT | - | - | 1 | - | - | - | - | - | |
| | 501+85 EC, RT | - | - | 1 | - | - | - | - | - | |
| | CENTER ISLAND | - | 1 | - | - | - | - | - | - | CENTER OF INTERSECTION |
| | 501+75 EC, RT | - | - | - | 1 | 1 | 49 | 37 | - | MOVE TO 501+95 EA, RT |
| CAT 0020 SUBTOTALS | | 0 | 1 | 4 | 1 | 1 | 49 | 37 | 0 | |
| PROJECT TOTALS | | 1 | 1 | 4 | 1 | 1 | 49 | 37 | 1 | |

VEGETATED POROUS PAVEMENT

| SPV.0165.01 VEGETATED POROUS PAVEMENT | | | |
|--|-------------|-------------|----------------------|
| CATEGORY | LOCATION | SF | REMARKS |
| 0010 | NW QUADRANT | 240 | TRUCK OVERTRACK AREA |
| | SW QUADRANT | 278 | TRUCK OVERTRACK AREA |
| | NE QUADRANT | 216 | TRUCK OVERTRACK AREA |
| | SE QUADRANT | 269 | TRUCK OVERTRACK AREA |
| PROJECT TOTAL | | 1003 | |

PULL BOX ITEMS

| CATEGORY | NO. | LOCATION | 653.0154 PULL BOXES NON-CONDUCTIVE 24x36-INCH EACH |
|----------------------|-----|-----------|--|
| 0010 | PB1 | SW CORNER | 1 |
| | PB2 | SE CORNER | 1 |
| PROJECT TOTAL | | | 2 |

LIGHTING UNIT ITEMS

| CATEGORY | LIGHTING UNIT NO. | 654.0105 CONCRETE BASES TYPE 5 EACH | 657.0255 TRANSFORMER BASES BREAKAWAY 11 1/2-INCH BOLT CIRCLE EACH | 657.0322 POLES TYPE 5 ALUMINUM EACH | 657.0710 LUMINAIRE ARMS TRUSS TYPE 4 1/2-INCH CLAMP 12-FT EACH | 659.1120 LUMINAIRES UTILITY LED B EACH |
|----------------------|-------------------|--|--|---|---|---|
| 0010 | A1 | 1 | 1 | 1 | 1 | 1 |
| | A2 | 1 | 1 | 1 | 1 | 1 |
| | A3 | 1 | 1 | 1 | 1 | 1 |
| | A4 | 1 | 1 | 1 | 1 | 1 |
| PROJECT TOTAL | | 4 | 4 | 4 | 4 | 4 |

LIGHTING ELECTRICAL WIRE AND CONDUIT ITEMS

| CATEGORY | STA. FROM | STA. TO | 652.0225 CONDUIT RIGID NONMETALLIC SCHEDULE 40 2-INCH (FT) | 655.0610 ELECTRICAL WIRE LIGHTING 12 AWG (FT) | 655.0620 ELECTRICAL WIRE LIGHTING 8 AWG (FT) |
|----------------------|-----------|---------|---|--|---|
| 0010 | LLC | A1 | 10 | 160 | 60 |
| | A1 | A2 | 64 | 160 | 222 |
| | LCC | PB1 | 48 | - | 174 |
| | PB1 | A4 | 97 | 160 | 321 |
| | A4 | PP2 | 52 | - | 186 |
| | PP2 | A3 | 84 | 160 | 282 |
| PROJECT TOTAL | | | 355 | 640 | 1245 |

LIGHTING CONTROL CABINET

| CATEGORY | CABINET | STATION | SPV.0060.06 CONCRETE CONTROL CABINET BASE TYPE SPECIAL EACH | SPV.0060.07 LIGHTING CONTROL CABINET TYPE SPECIAL EACH |
|----------------------|---------|---------|---|--|
| 0010 | LCC | 35+53 | 1 | 1 |
| PROJECT TOTAL | | | 1 | 1 |

| | | |
|--|----------------------|-------------------|
| R/W PROJECT NUMBER 5991-02-61 | SHEET NUMBER 4.01 | TOTAL SHEETS 2 |
| R/W PROJECT NUMBER | | |
| PLAT OF RIGHT OF WAY REQUIRED FOR CITY OF ONALASKA, EAST AVENUE N (RIDERS CLUB ROAD INTERSECTION) | | |
| EAST AVE NORTH | | LACROSSE COUNTY |

CAUTION
THIS PLAT IS FOR ILLUSTRATIVE PURPOSES ONLY. DEEDS MUST BE CHECKED TO DETERMINE PROPERTY BOUNDARIES.

NOTES:
POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), LACROSSE COUNTY, NAD 83 (2011) IN US SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.
RIGHT OF WAY MONUMENTS ARE 3/4"X 24" REBAR AND ARE PLACED PRIOR TO OR AT THE TIME OF LAND TITLE TRANSFER.
RIGHT OF WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY OR OTHER SURVEYS OF PUBLIC RECORD.

CONVENTIONAL SYMBOLS

| | | | | | |
|---|-----|---------------------------|--|--------------------------------------|----|
| SECTION LINE | --- | SECTION CORNER SYMBOL | | R/W MONUMENT (TO BE SET) | ● |
| QUARTER LINE | --- | SECTION CORNER MONUMENT | | NON-MONUMENTED R/W POINT | ○ |
| SIXTEENTH LINE | --- | GEODETIC SURVEY MONUMENT | | FOUND IRON PIN (1-INCH UNLESS NOTED) | IP |
| NEW REFERENCE LINE | --- | SIXTEENTH CORNER MONUMENT | | OFF-PREMISE SIGN | |
| NEW R/W LINE | --- | SIGN | | COMPENSABLE | |
| EXISTING R/W OR HE LINE | --- | PARCEL NUMBER | | NON-COMPENSABLE | |
| PROPERTY LINE | --- | UTILITY NUMBER | | | |
| LOT, TIE & OTHER MINOR LINES | --- | PARALLEL OFFSETS | | | |
| SLOPE INTERCEPT | --- | | | | |
| CORPORATE LIMITS | --- | | | | |
| UNDERGROUND FACILITY (COMMUNICATIONS, ELECTRIC, ETC) | --- | | | | |
| NEW R/W (FEE OR HE) (HATCHING VARIES BY OWNER) | --- | | | | |
| TEMPORARY LIMITED EASEMENT AREA | --- | | | | |
| EASEMENT AREA (PERMANENT LIMITED OR RESTRICTED DEVELOPMENT) | --- | | | | |
| TRANSMISSION STRUCTURES | --- | | | | |
| BUILDING TO BE REMOVED | --- | | | | |
| BRIDGE | --- | | | | |
| CULVERT | --- | | | | |

CONVENTIONAL UTILITY SYMBOLS

| | |
|-----------------------------|-----|
| WATER | --- |
| GAS | --- |
| TELEPHONE | --- |
| OVERHEAD TRANSMISSION LINES | --- |
| ELECTRIC | --- |
| CABLE TELEVISION | --- |
| FIBER OPTIC | --- |
| SANITARY SEWER | --- |
| STORM SEWER | --- |
| ELECTRIC TOWER | --- |

CONVENTIONAL ABBREVIATIONS

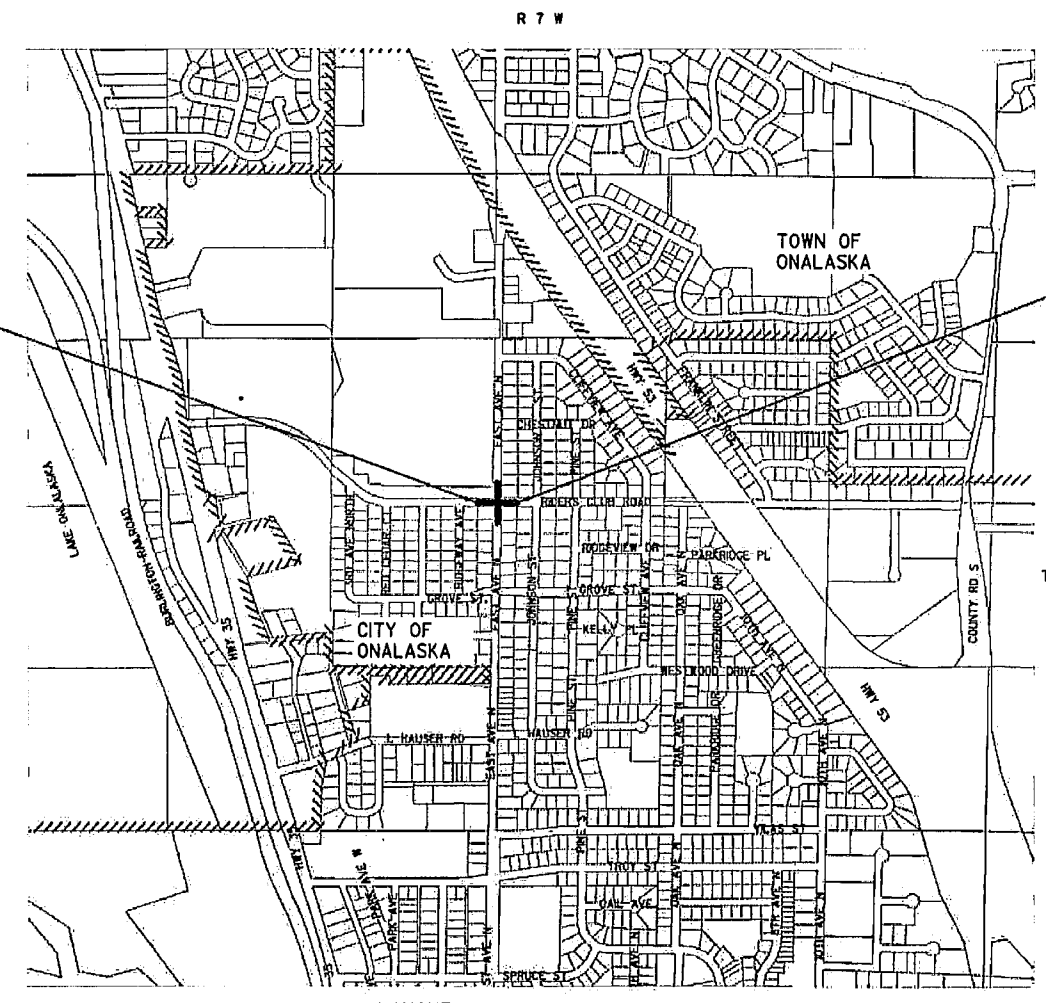
| | | | |
|----------------------------|-------|-------------------------|--------|
| ACCESS RIGHTS | AR | POINT OF INTERSECTION | PI |
| ACRES | AC | PROPERTY LINE | PL |
| AHEAD | AH | RECORDED AS | (100') |
| ALUMINUM | ALUM | REEL / IMAGE | R/I |
| AND OTHERS | ET AL | REFERENCE LINE | R/L |
| BACK | BK | REMAINING | REM |
| BLOCK | BLK | RESTRICTIVE DEVELOPMENT | RDE |
| CENTERLINE | C/L | EASEMENT | |
| CERTIFIED SURVEY MAP | CSM | RIGHT | RT |
| CONCRETE | CONC | RIGHT OF WAY | R/W |
| COUNTY | CO | SECTION | SEC |
| COUNTY TRUNK HIGHWAY | CTH | SEPTIC VENT | SEP V |
| DISTANCE | DIST | SQUARE FEET | SF |
| CORNER | COR | STATE TRUNK HIGHWAY | STH |
| DOCUMENT NUMBER | DOC | STATION | STA |
| EASEMENT | EASE | TELEPHONE PEDESTAL | TP |
| EXISTING | EX | TEMPORARY LIMITED | TLE |
| GAS VALVE | GV | EASEMENT | |
| GRID NORTH | GN | TRANSPORTATION PROJECT | TPP |
| HIGHWAY EASEMENT | HE | PLAT | |
| IDENTIFICATION | ID | UNITED STATES HIGHWAY | USH |
| LAND CONTRACT | LC | VOLUME | V |
| LEFT | LT | | |
| MONUMENT | MON | | |
| NATIONAL GEODETIC SURVEY | NGS | | |
| NUMBER | NO | | |
| OUTLOT | OL | | |
| PAGE | P | | |
| POINT OF TANGENCY | PT | | |
| PERMANENT LIMITED EASEMENT | PLE | | |
| POINT OF BEGINNING | POB | | |
| POINT OF CURVATURE | PC | | |
| POINT OF COMPOUND CURVE | PCC | | |

CURVE DATA

| | |
|--------------------|---------|
| LONG CHORD | LCH |
| LONG CHORD BEARING | LCB |
| RADIUS | R |
| DEGREE OF CURVE | D |
| CENTRAL ANGLE | Δ/DELTA |
| LENGTH OF CURVE | L |
| TANGENT | T |
| DIRECTION AHEAD | DA |
| DIRECTION BACK | DB |

BEGIN RELOCATION ORDER
STA 34+00
Y=165617.768
X=449265.102
28.041 FEET SOUTH AND 1561.983 FEET WEST OF THE EAST 1/4 CORNER OF SECTION 32, T17N, R7W

END RELOCATION ORDER
STA 38+00
Y=165624.995
X=449665.037
20.814 FEET SOUTH AND 1162.048 FEET EAST OF THE EAST 1/4 CORNER OF SECTION 32, T17N, R7W



LAYOUT
SCALE 0 1500 FT
TOTAL NET LENGTH OF CENTERLINE = 0.076 Miles

ORIGINAL PLAT PREPARED BY

11-16-2021 (Date) (Signature)

REVISION DATE
2-15-2022 N.C.

CITY OF ONALASKA

APPROVED FOR THE CITY
DATE: 11-18-2021 (Signature)

| SCHEDULE OF LANDS & INTERESTS REQUIRED | | | | | | |
|--|----------------------------------|-------------------|------------|-----------------|--------------|--------|
| PARCEL NUMBER | OWNERS | INTEREST REQUIRED | R/W NEW SF | R/W EXISTING SF | R/W TOTAL SF | TLE SF |
| 1 | SCHOOL DISTRICT OF ONALASKA | FEE/TLE | 4 | -- | 4 | 772 |
| 2 | KURT J & COURTNEY M VANAUKEN | FEE/TLE | 17 | -- | 17 | 700 |
| 3 | SUSAN M BAKER | FEE/TLE | 56 | -- | 56 | 976 |
| 4 | KENT M & STEPHANIE N MANGLITZ | FEE/TLE | 48 | -- | 48 | 2077 |
| 10 | CITY OF ONALASKA | RELEASE OF RIGHTS | | | | |
| 11 | TREMPEALEAU ELECTRIC COOPERATIVE | RELEASE OF RIGHTS | | | | |
| 12 | DAIRYLAND POWER COOPERATIVE | RELEASE OF RIGHTS | | | | |
| 13 | CHARTER COMMUNICATIONS | RELEASE OF RIGHTS | | | | |
| 14 | RIVERLAND ENERGY | RELEASE OF RIGHTS | | | | |
| 15 | CENTURYLINK | RELEASE OF RIGHTS | | | | |
| 16 | CITY OF ONALASKA (FIBER OPTIC) | RELEASE OF RIGHTS | | | | |

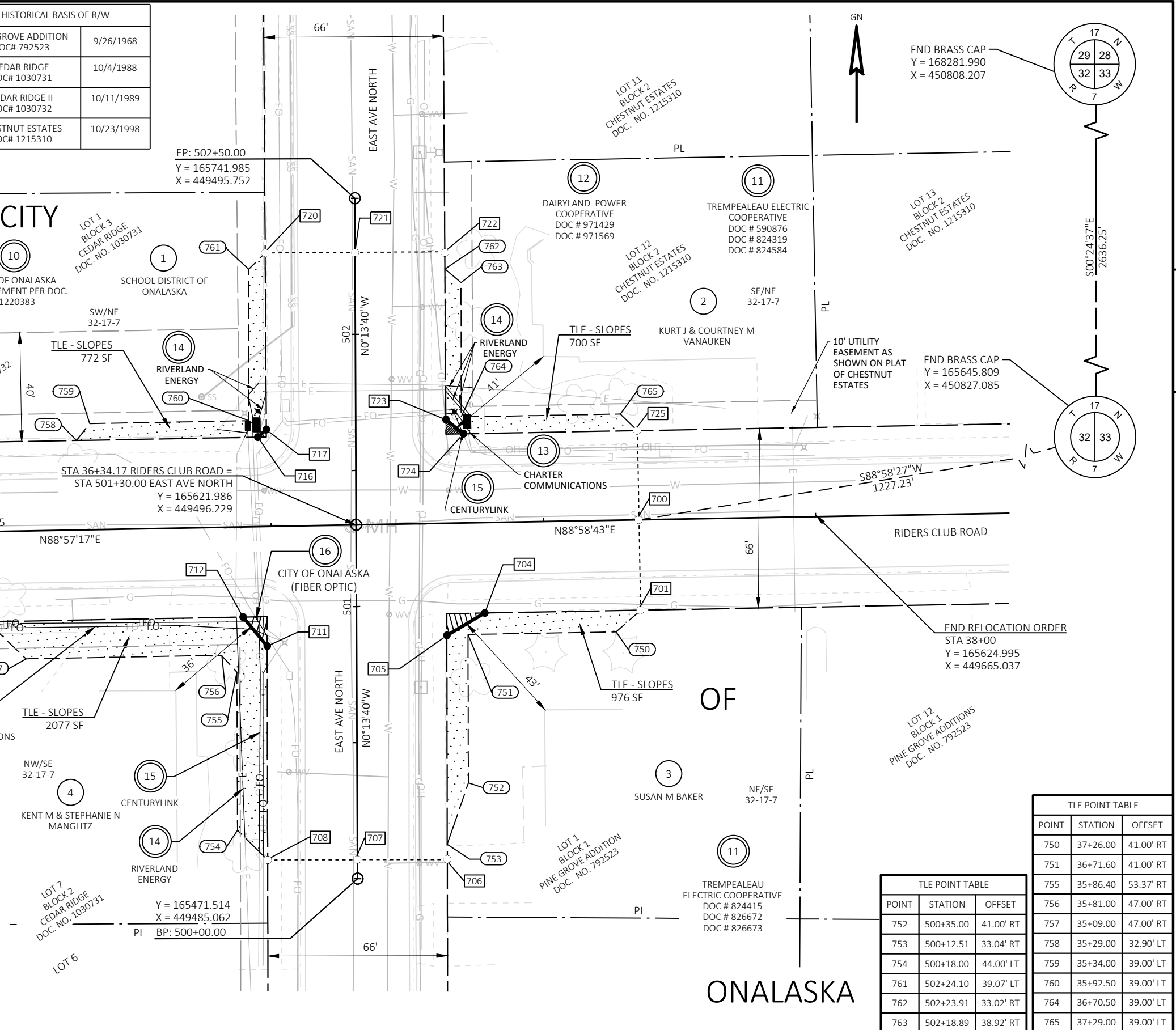
| HISTORICAL BASIS OF R/W | |
|---------------------------------|------------|
| PINE GROVE ADDITION DOC# 792523 | 9/26/1968 |
| CEDAR RIDGE DOC# 1030731 | 10/4/1988 |
| CEDAR RIDGE II DOC# 1030732 | 10/11/1989 |
| CHESTNUT ESTATES DOC# 1215310 | 10/23/1998 |

OWNERS NAMES SHOW FOR REFERENCE PURPOSED ONLY AND SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE DEPARTMENT

| COURSE | BEARING | DISTANCE |
|--------|-------------------|----------|
| 700 | 701 S01° 01' 16"E | 33.20' |
| 701 | 704 S88° 57' 31"W | 57.16' |
| 704 | 705 S59° 16' 05"W | 16.19' |
| 705 | 706 S00° 07' 24"E | 82.21' |
| 706 | 707 S89° 46' 21"W | 33.03' |
| 707 | 708 S89° 46' 21"W | 32.97' |
| 708 | 711 N00° 07' 24"W | 78.56' |
| 711 | 712 N39° 37' 08"W | 13.89' |
| 712 | 713 S88° 57' 34"W | 94.05' |
| 713 | 714 N01° 02' 44"W | 33.10' |
| 714 | 715 N01° 02' 44"W | 32.90' |
| 715 | 716 N88° 57' 34"E | 100.65' |
| 716 | 717 N48° 06' 37"E | 4.20' |
| 717 | 720 N00° 22' 00"W | 64.91' |
| 720 | 721 N89° 55' 29"E | 32.89' |
| 721 | 722 N89° 55' 29"E | 33.11' |
| 722 | 723 S00° 22' 00"E | 61.35' |
| 723 | 724 S50° 55' 16"E | 8.26' |
| 724 | 725 N88° 57' 31"E | 63.83' |
| 725 | 700 S01° 01' 16"E | 32.80' |

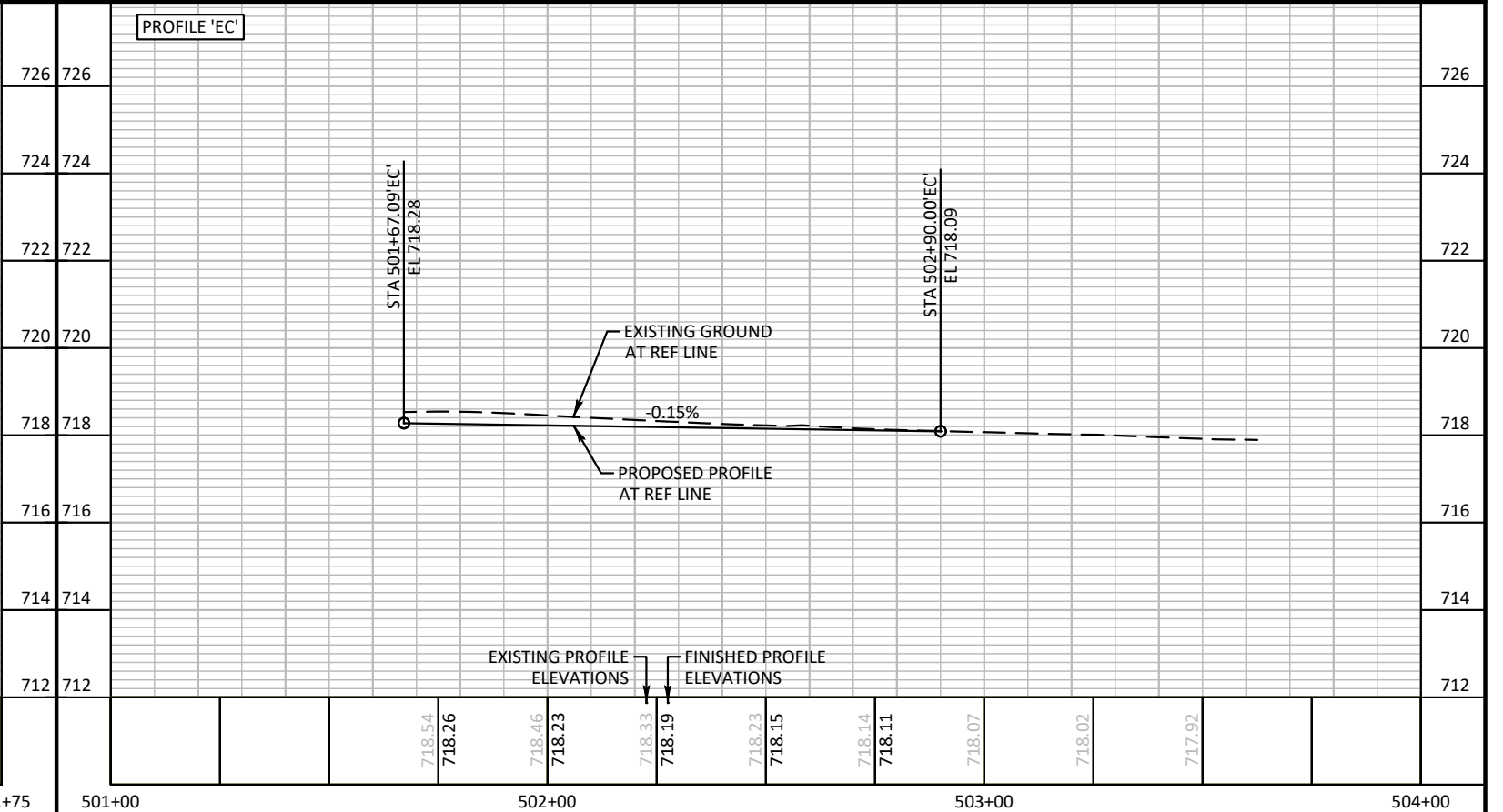
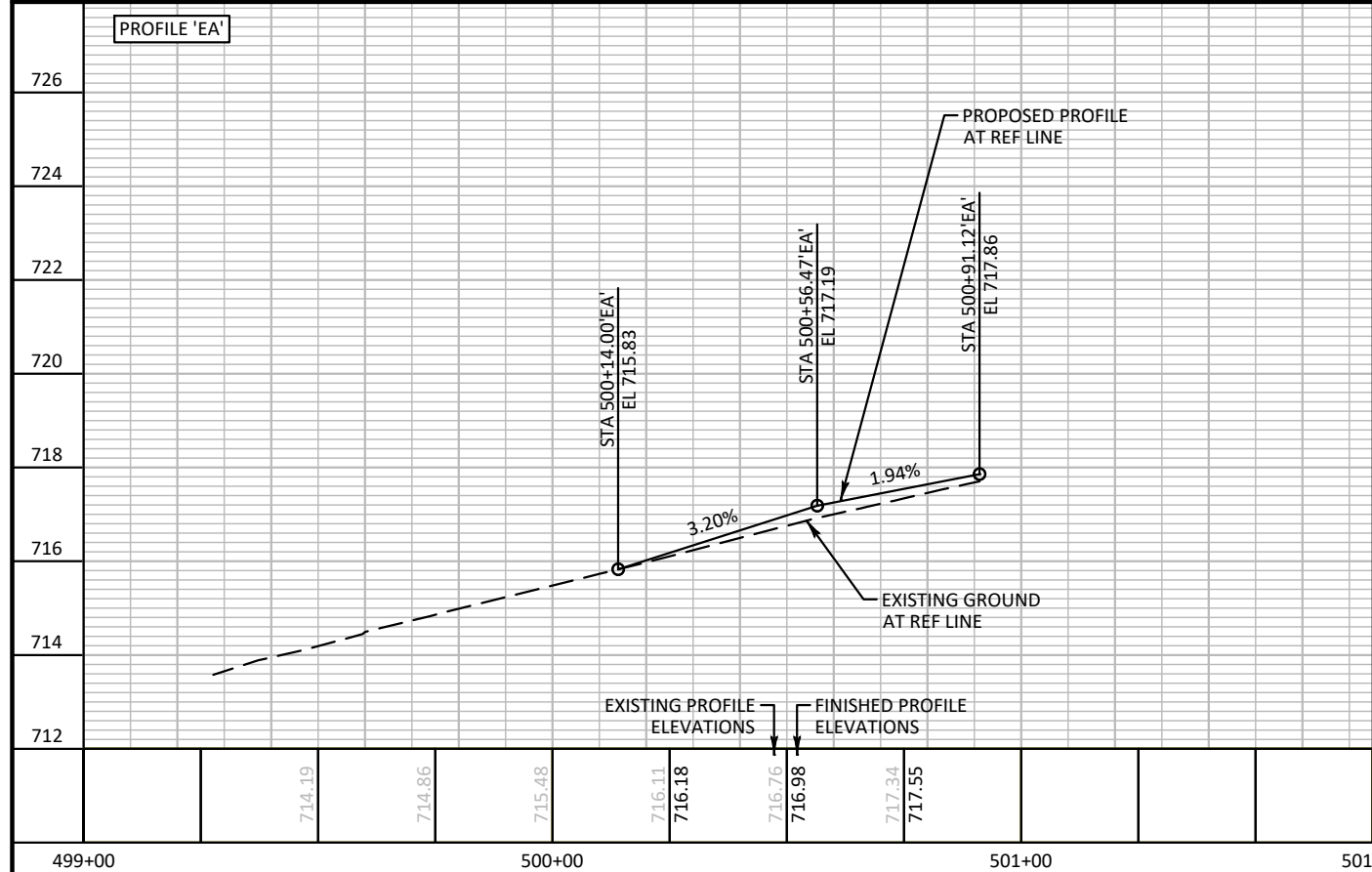
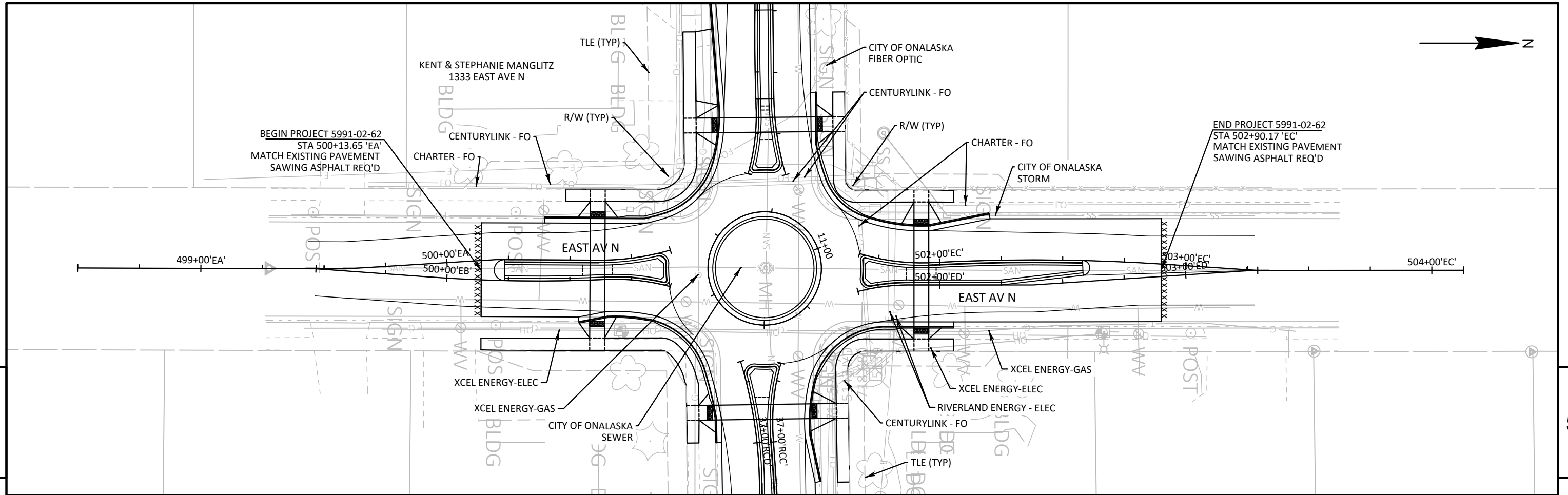
| FEE STATION & OFFSET TABLE | | | | |
|----------------------------|----------|-----------|------------|------------|
| POINT | STATION | OFFSET | Y COORDS | X COORDS |
| 700 | 37+35.18 | 0.00' RT | 165623.837 | 449600.047 |
| 701 | 37+35.19 | 33.20' RT | 165590.640 | 449600.639 |
| 704 | 36+77.84 | 33.22' RT | 165589.601 | 449543.488 |
| 705 | 36+63.78 | 41.25' RT | 165581.328 | 449529.571 |
| 711 | 35+97.71 | 43.97' RT | 165577.415 | 449463.579 |
| 712 | 35+89.05 | 33.11' RT | 165588.114 | 449454.722 |
| 713 | 34+95.00 | 33.10' RT | 165586.406 | 449360.690 |
| 714 | 34+95.00 | 0.00' RT | 165619.501 | 449360.086 |
| 715 | 34+95.01 | 32.90' LT | 165652.395 | 449359.486 |
| 716 | 35+95.65 | 32.89' LT | 165654.223 | 449460.122 |
| 724 | 36+71.17 | 35.64' LT | 165655.470 | 449535.645 |
| 725 | 37+35.17 | 32.80' LT | 165656.630 | 449599.462 |

| FEE STATION & OFFSET TABLE | | | | |
|----------------------------|-----------|-----------|------------|------------|
| POINT | STATION | OFFSET | Y COORDS | X COORDS |
| 706 | 500+07.00 | 33.03' RT | 165499.118 | 449529.748 |
| 707 | 500+07.00 | 0.00' LT | 165498.987 | 449496.717 |
| 708 | 500+07.00 | 32.97' LT | 165498.856 | 449463.749 |
| 717 | 501+65.18 | 32.84' LT | 165657.030 | 449463.252 |
| 720 | 502+30.09 | 32.99' LT | 165721.940 | 449462.836 |
| 721 | 502+30.00 | 0.00' LT | 165721.983 | 449495.831 |
| 722 | 502+29.91 | 33.01' RT | 165722.026 | 449528.837 |
| 723 | 501+68.56 | 33.15' RT | 165660.680 | 449529.230 |

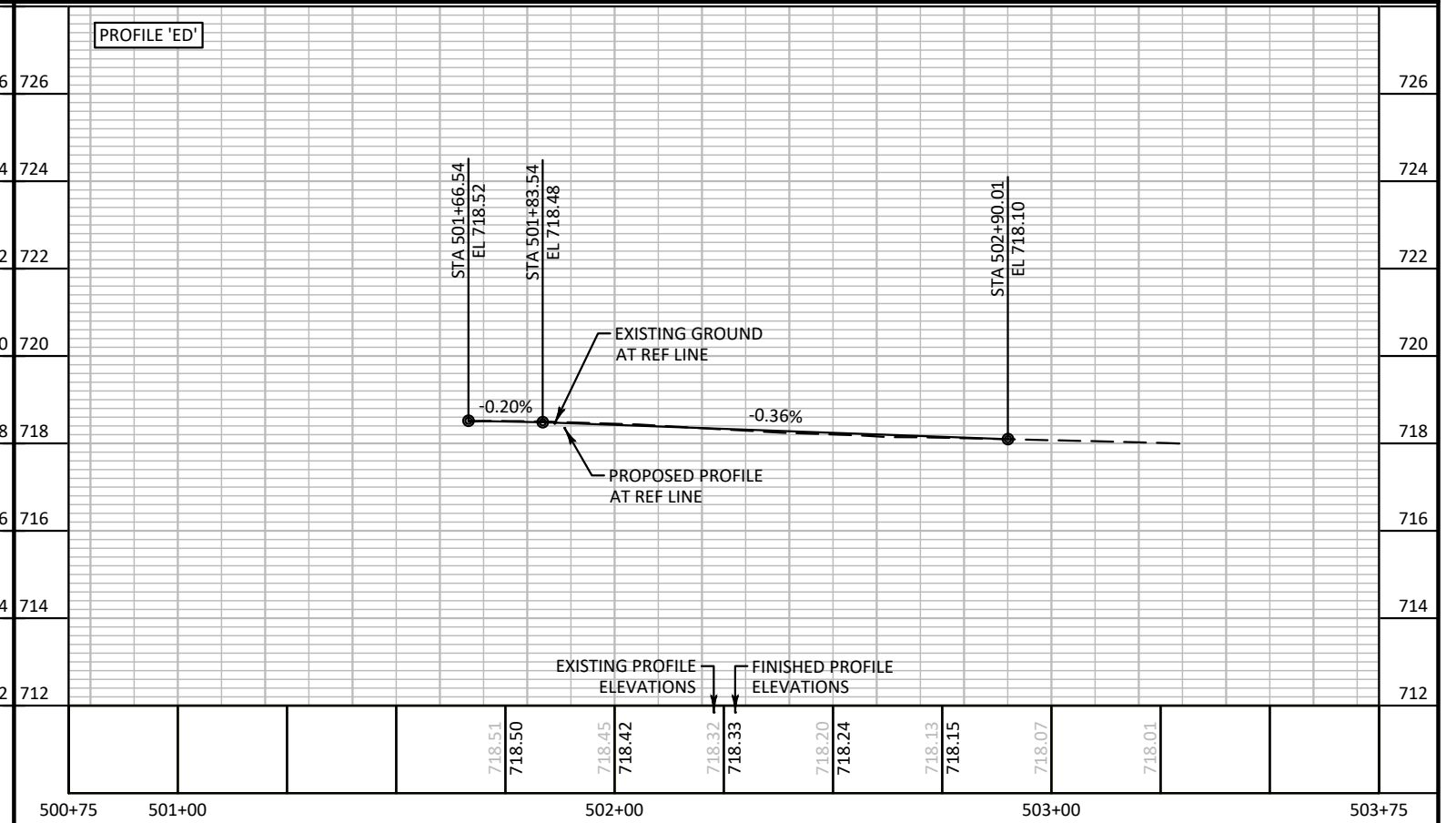
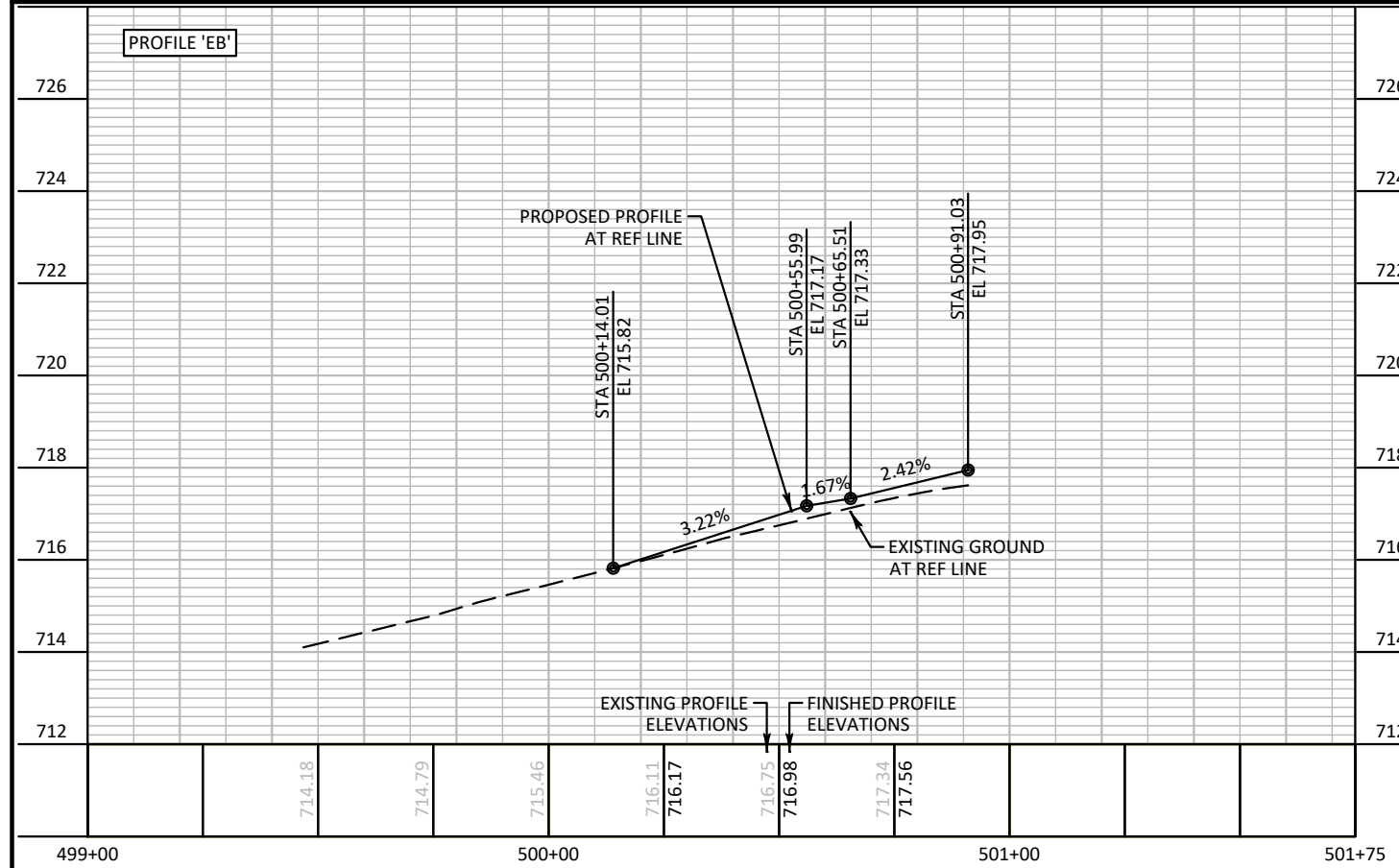
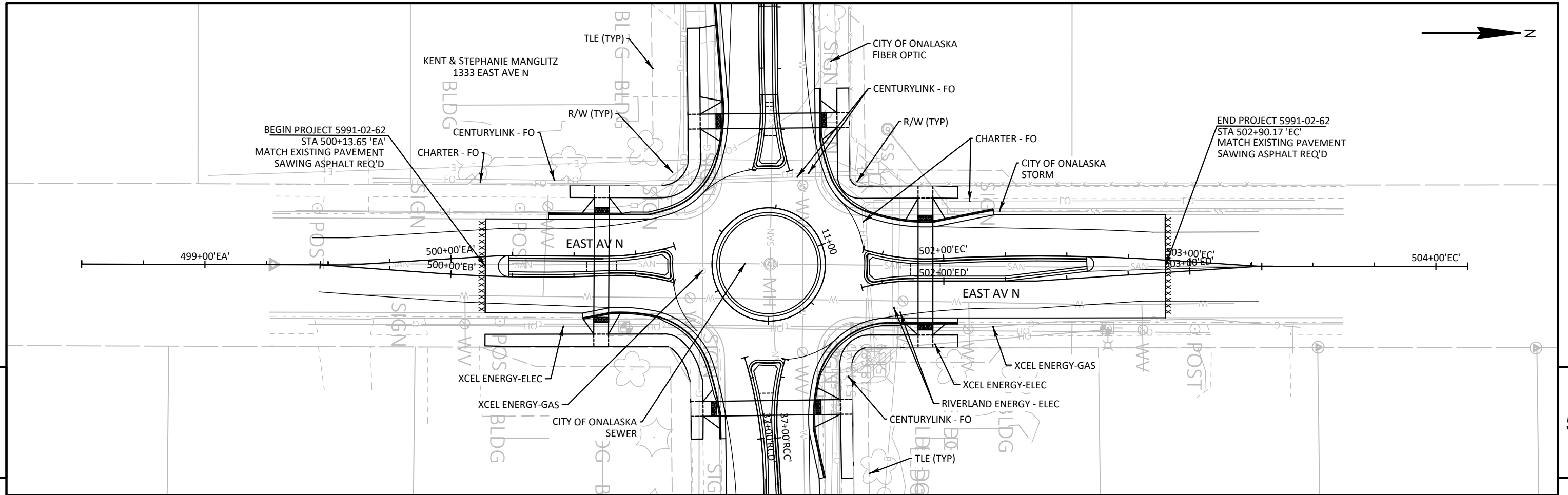


| TLE POINT TABLE | | |
|-----------------|----------|-----------|
| POINT | STATION | OFFSET |
| 750 | 37+26.00 | 41.00' RT |
| 751 | 36+71.60 | 41.00' RT |
| 755 | 35+86.40 | 53.37' RT |
| 756 | 35+81.00 | 47.00' RT |
| 757 | 35+09.00 | 47.00' RT |
| 758 | 35+29.00 | 32.90' LT |
| 759 | 35+34.00 | 39.00' LT |
| 760 | 35+92.50 | 39.00' LT |
| 764 | 36+70.50 | 39.00' LT |
| 765 | 37+29.00 | 39.00' LT |

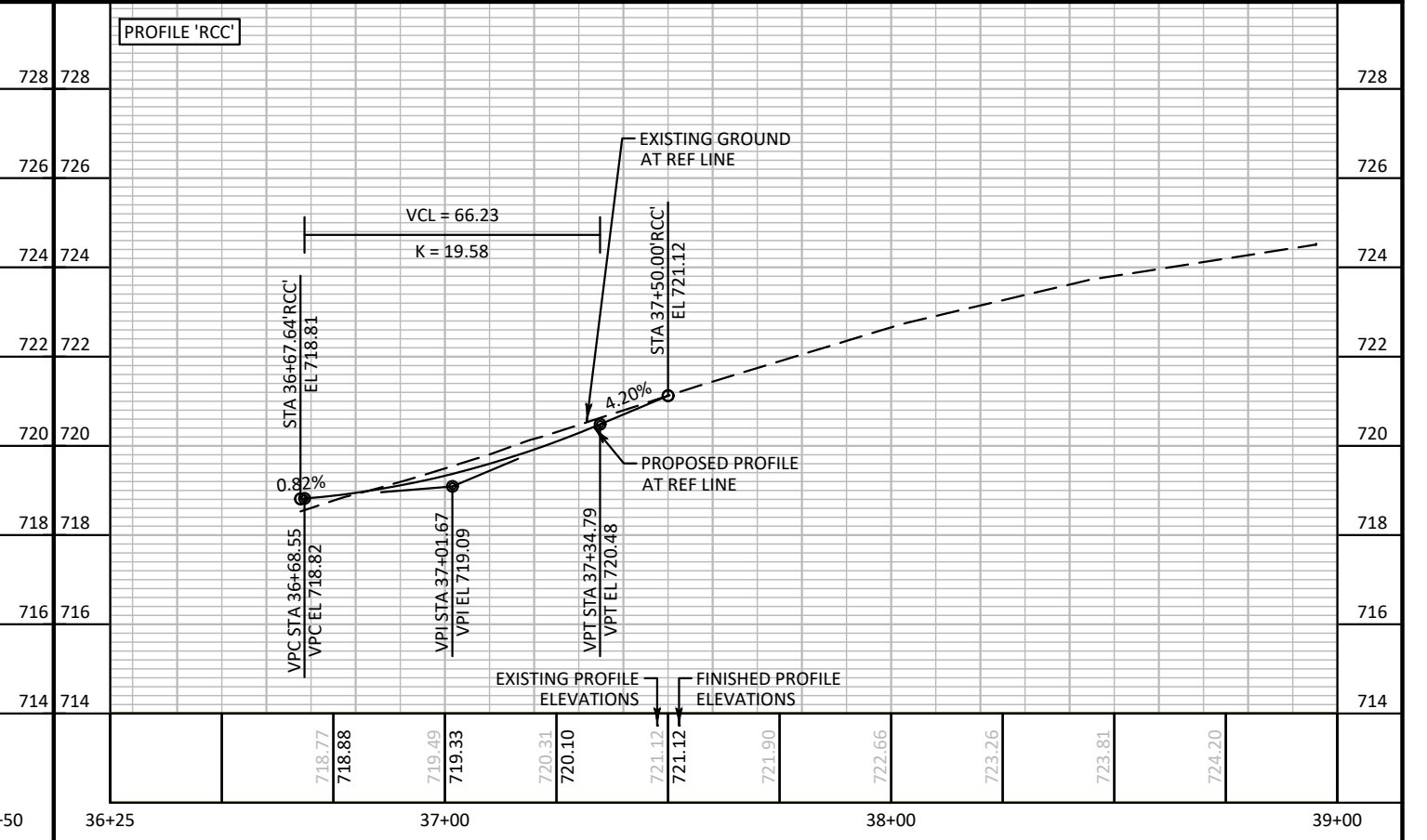
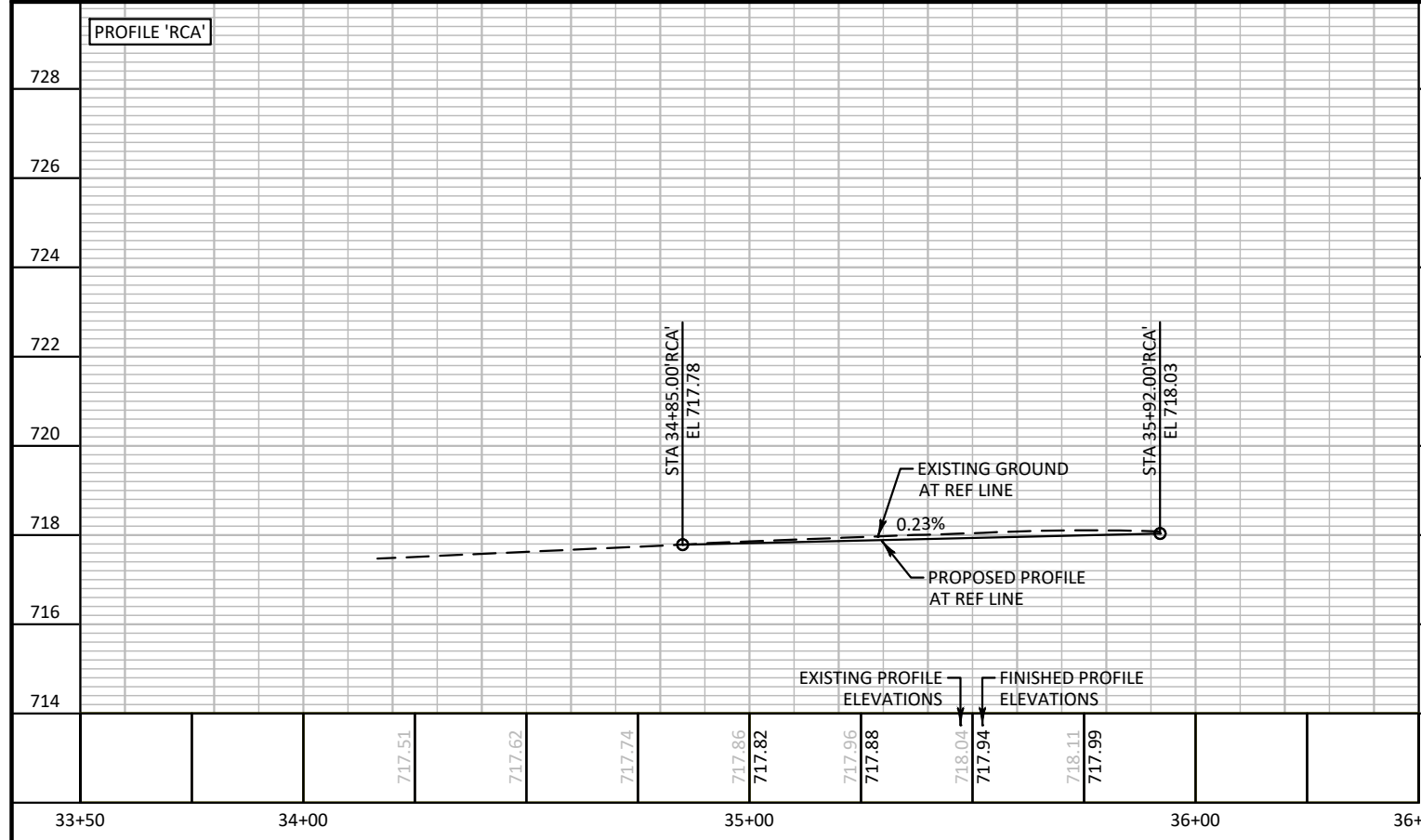
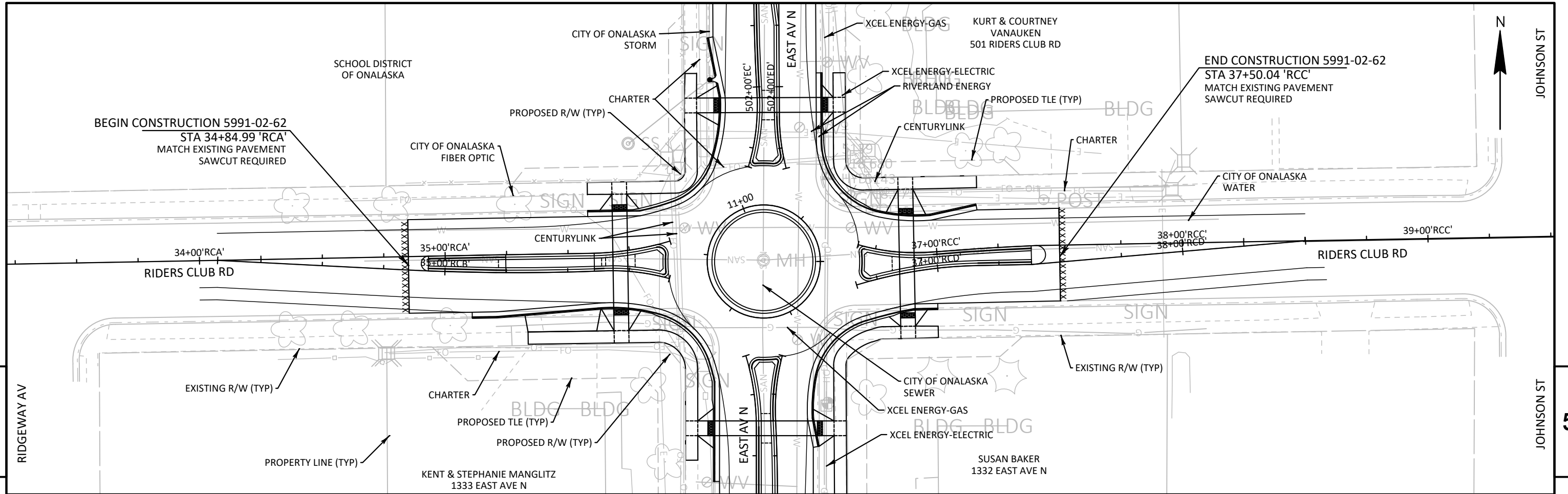
| | | | | | |
|--------------------------|--------------------|------------------------|---------------------|---|--------------------|
| REVISION DATE 2-15-22 | DATE | SCALE, FEET 0 20 40 | HWY: RIDERS CLUB RD | STATE R/W PROJECT NUMBER 5991-02-61 | PLAT SHEET 4.02 |
| | GRID FACTOR N/A | | COUNTY: LACROSSE | CONSTRUCTION PROJECT NUMBER 5991-02-62 | PS&E SHEET |



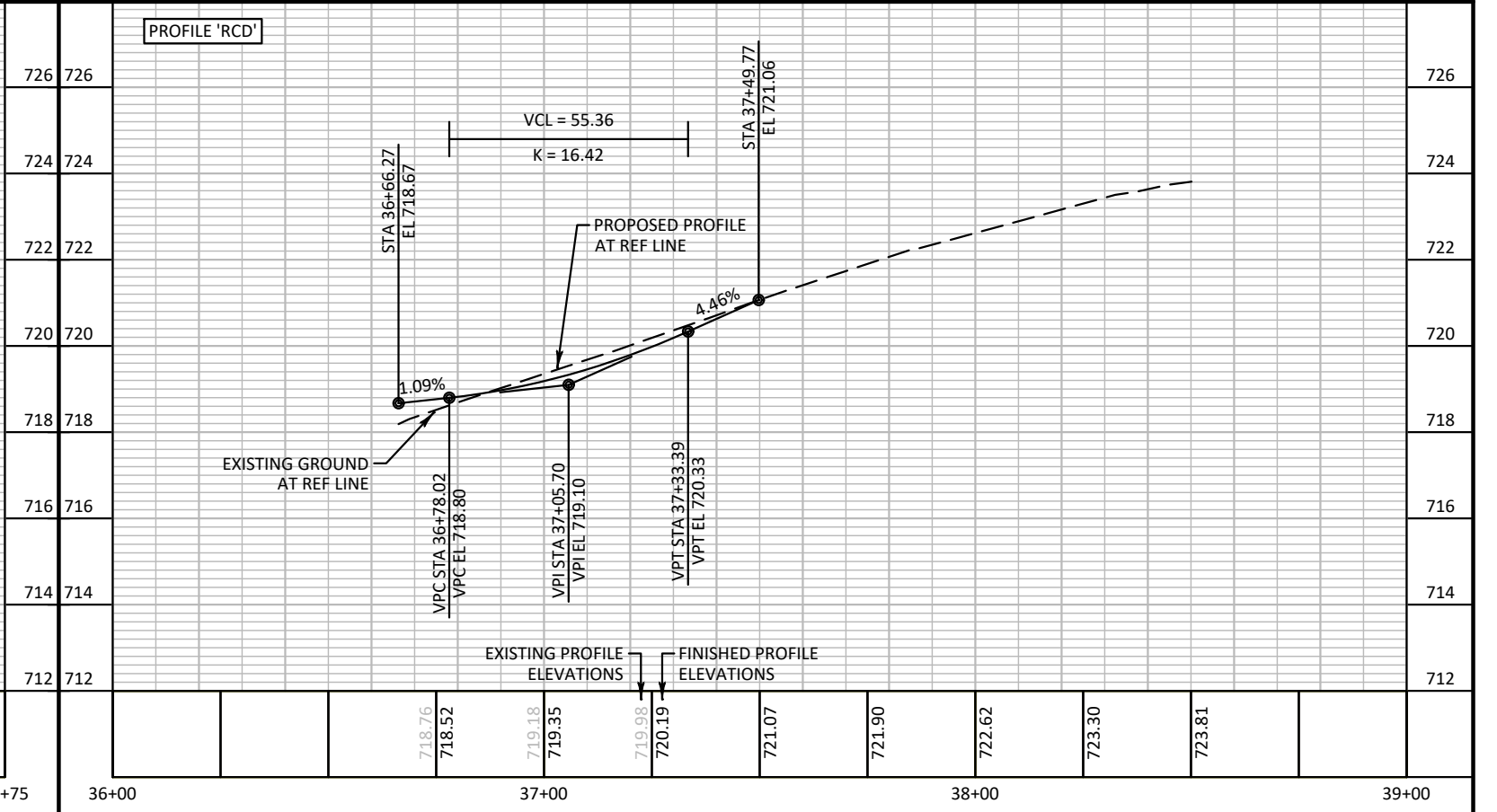
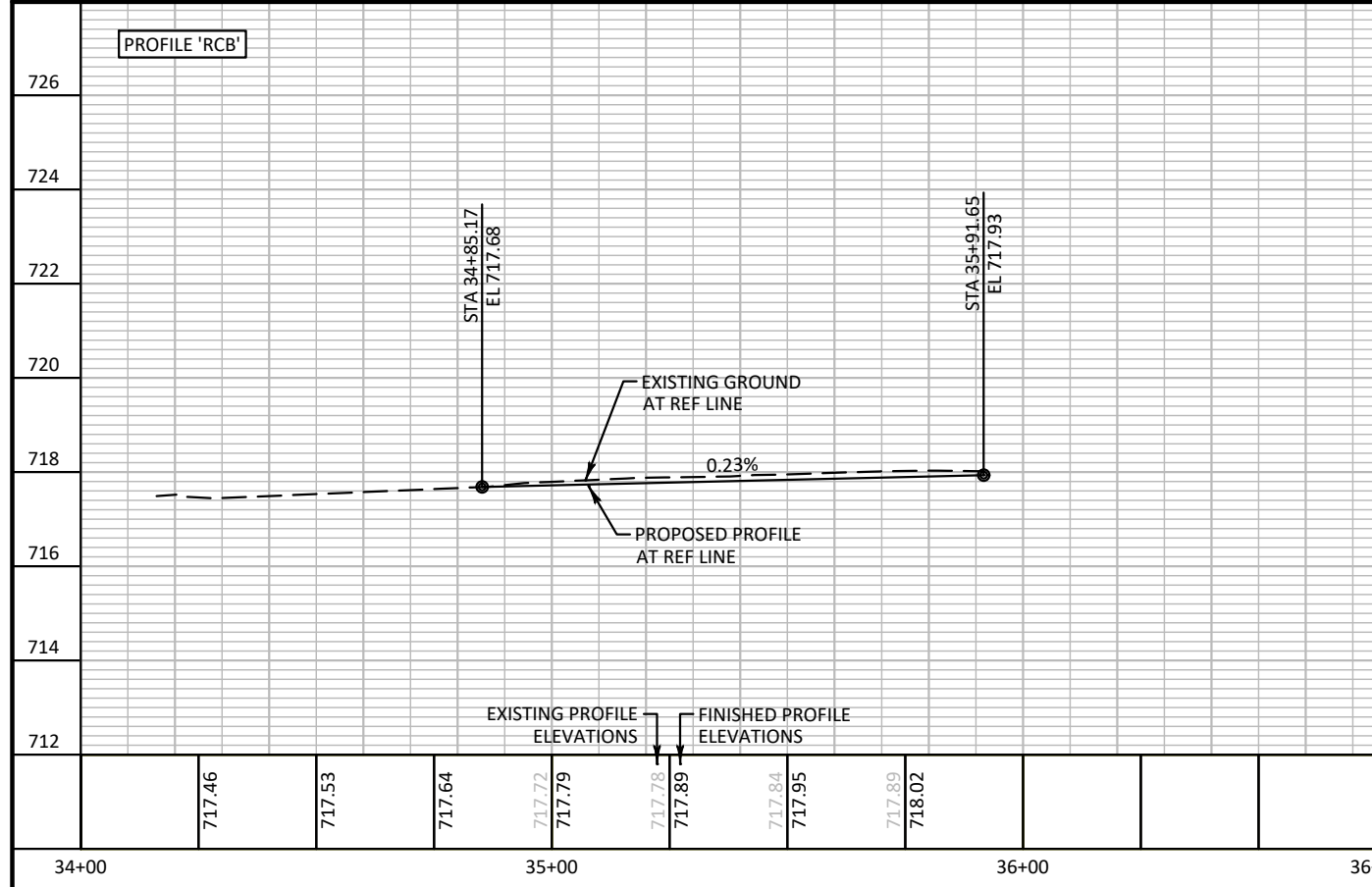
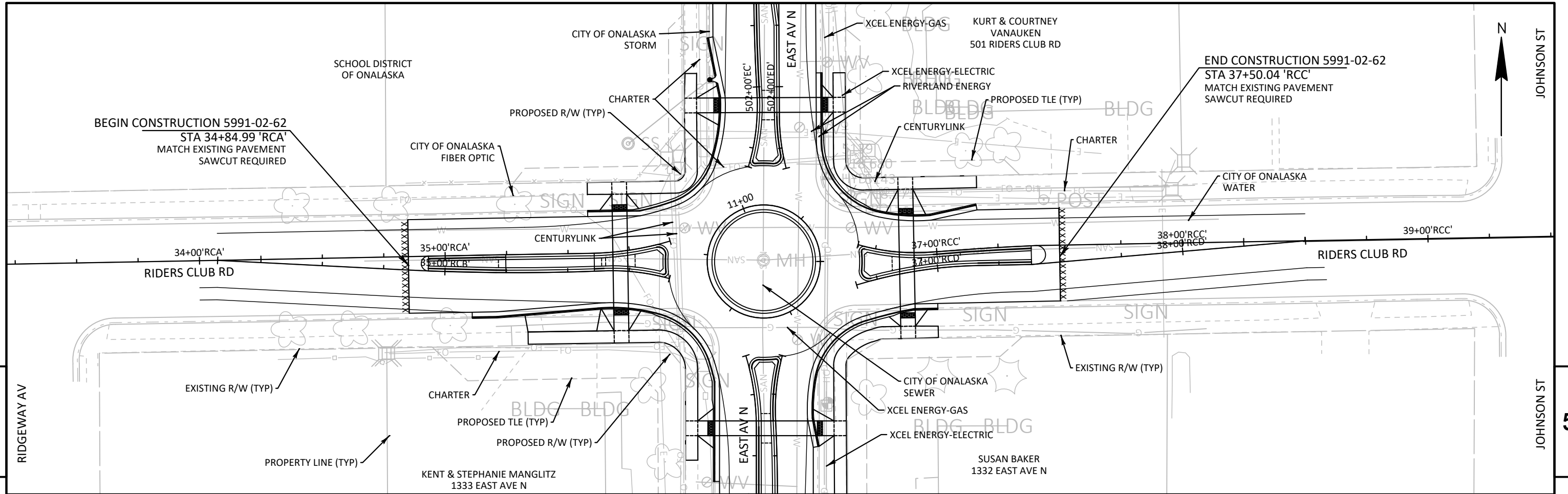
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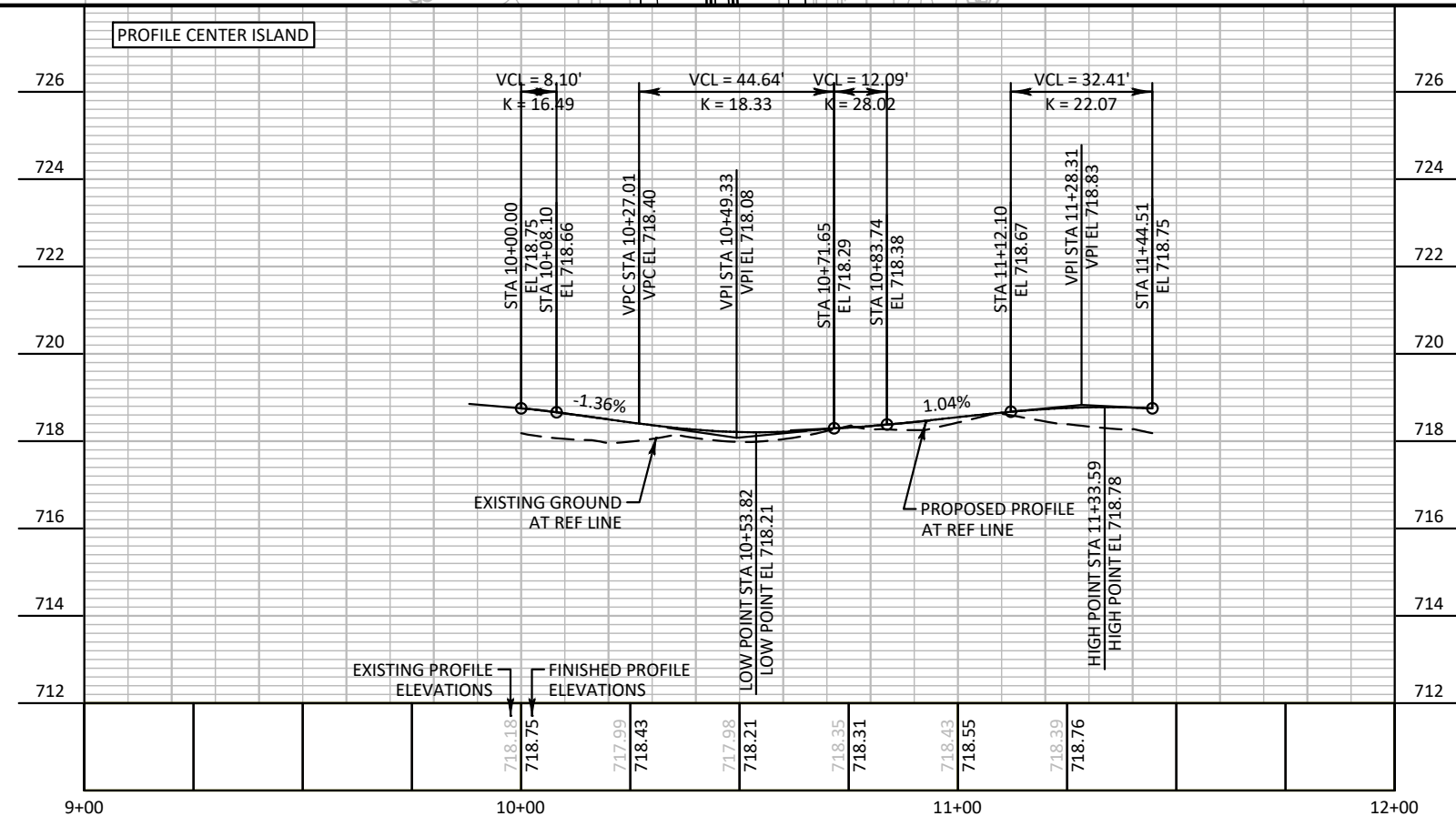
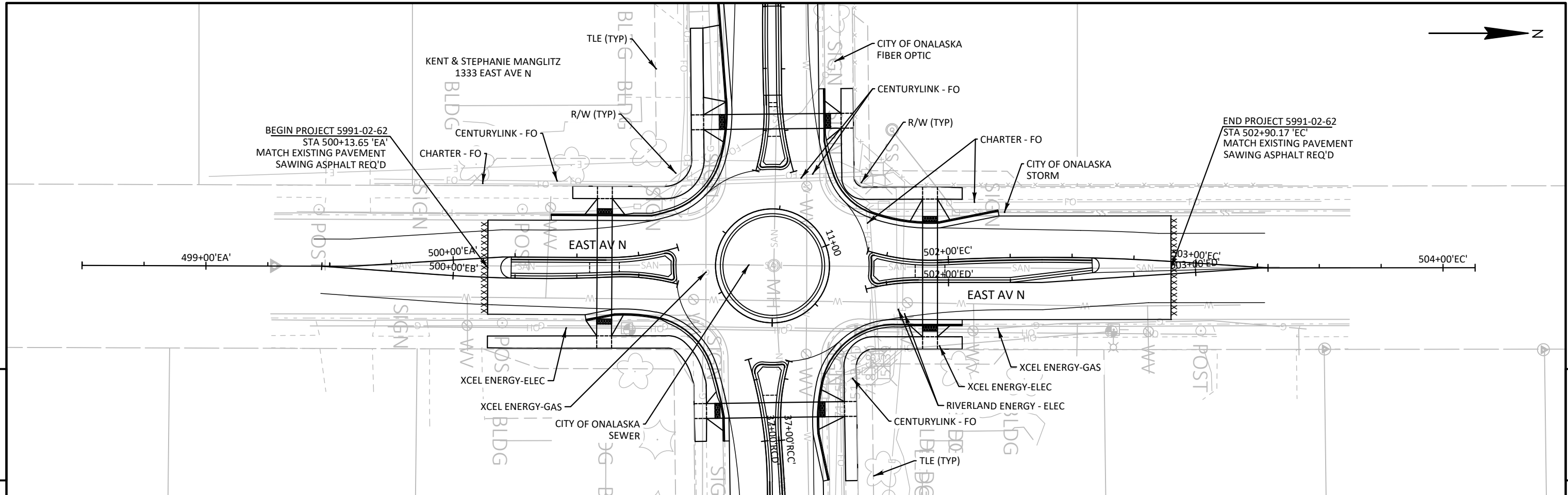
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PROJECT NO: 5991-02-62 HWY: EAST AVENUE N COUNTY: LA CROSSE PLAN AND PROFILE: RIDERS CLUB RD SHEET: E

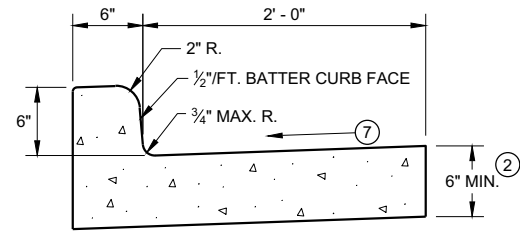


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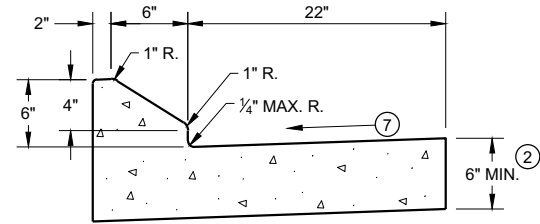


Standard Detail Drawing List

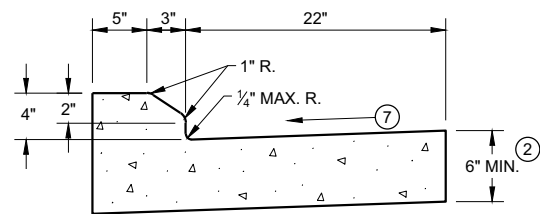
| | |
|-----------|--|
| 08D01-22A | CONCRETE CURB & GUTTER |
| 08D01-22B | CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS |
| 08D05-20A | CURB RAMPS TYPES 1 AND 1-A |
| 08D05-20B | CURB RAMPS TYPES 2 AND 3 |
| 08D05-20C | CURB RAMPS TYPES 4A AND 4A1 |
| 08D05-20D | CURB RAMPS TYPE 4B AND 4B1 |
| 08D05-20E | CURB RAMPS TYPES 5, 6, 7A, 7B & 8 |
| 08D05-20F | CURB RAMPS RADIAL DETECTABLE WARNING FIELD APPLICATIONS |
| 08D05-20G | CURB RAMPS RECTANGULAR AND RADIAL DETECTABLE WARNING PLATES |
| 08D18-03 | DRIVEWAY AND SIDEWALK RAMPS TYPES X & Y |
| 08E10-02 | INLET PROTECTION TYPE A, B, C AND D |
| 09B02-10 | CONDUIT |
| 09B16-02 | PULL BOX NON-CONDUCTIVE |
| 09C02-09 | CONCRETE BASES, TYPES 1, 2, 5, & 6 |
| 09C03-04 | TRANSFORMER/PEDESTAL BASES |
| 09E01-15D | POLE MOUNTINGS FOR LIGHTING UNITS, TYPE 5 (30 FEET) |
| 09E01-15G | HARDWARE DETAILS FOR POLE MOUNTINGS |
| 09E03-06 | NON-FREEWAY LIGHTING UNIT POLE WIRING |
| 10A07-03 | ELECTRICAL DETAILS GROUND MOUNT LIGHT POLES PHASE-TO-PHASE SYSTEMS |
| 11B02-02 | CONCRETE MEDIAN NOSE |
| 13C19-03 | HMA LONGITUDINAL JOINTS |
| 15C02-08A | BARRICADES AND SIGNS FOR MAINLINE CLOSURES |
| 15C02-08B | BARRICADES AND SIGNS FOR VARIOUS CLOSURES |
| 15C02-08C | DETOUR SIGNING FOR MAINLINE CLOSURES |
| 15C07-15A | PAVEMENT MARKING SYMBOLS |
| 15C07-15C | PAVEMENT MARKING ARROWS |
| 15C07-15E | PAVEMENT MARKING FOR BIKE LANES |
| 15C08-22A | LONGITUDINAL MARKING (MAINLINE) |
| 15C18-06A | MEDIAN ISLAND MARKING PAVEMENT MARKINGS |
| 15C18-06B | MEDIAN ISLAND MARKING MEDIAN ISLAND NOSE |
| 15D30-07A | TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION |
| 15D30-07B | TRAFFIC CONTROL, TEMPORARY ADA COMPLIANT PEDESTRIAN ACCOMMODATION |
| 15D30-07C | TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION |
| 15D30-07F | TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION |



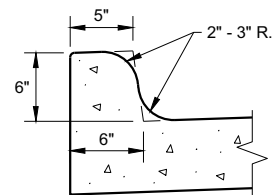
TYPES A^① & D



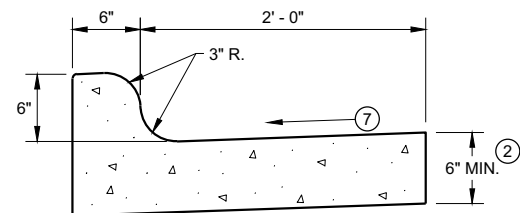
6" SLOPED CURB TYPES G^① & J



4" SLOPED CURB TYPES G^① & J

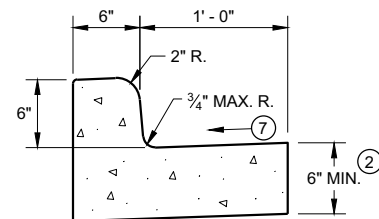


TYPES K^① & L
(OPTIONAL CURB SHAPE)



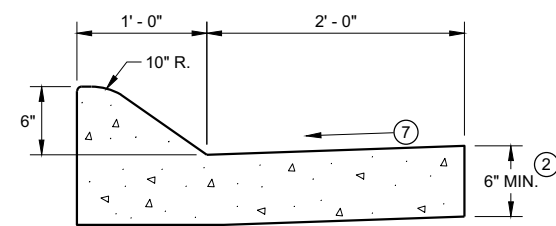
TYPES K^① & L

CONCRETE CURB AND GUTTER 30"

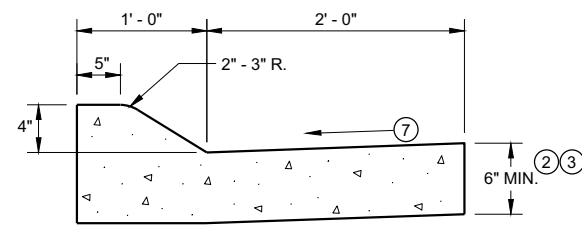


TYPES A^① & D

CONCRETE CURB AND GUTTER 18"

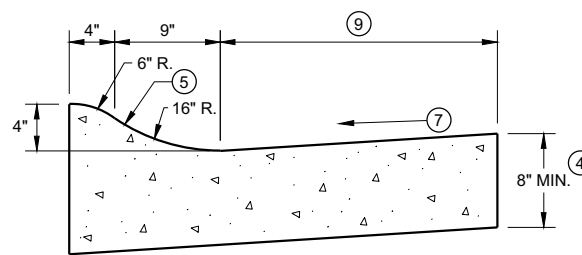


6" SLOPED CURB TYPES A^① & D



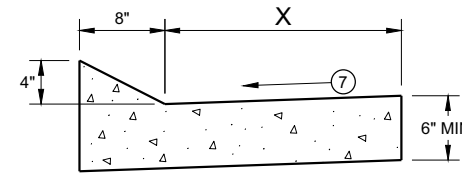
4" SLOPED CURB TYPES A^① & D

CONCRETE CURB AND GUTTER 36"



4" SLOPED CURB TYPES R^① & T

| | |
|------------|-----|
| TBT & TBTT | X |
| 30" | 22" |
| 36" | 28" |

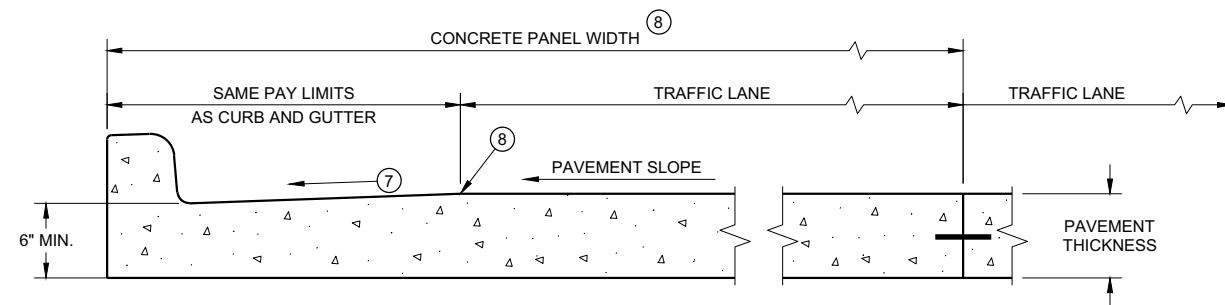


TYPES TBT & TBTT^①

CONCRETE CURB AND GUTTER

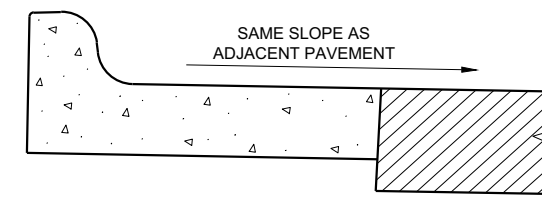
PAVEMENT THICKNESS AND MAXIMUM CONCRETE PANEL WIDTH TABLE

| PAVEMENT THICKNESS | MAXIMUM PANEL WIDTH |
|--------------------|---------------------|
| LESS THAN 10" | 12' |
| 10" & ABOVE | 15' |



PARTIAL SECTION OF PAVEMENT *
WITH INTEGRAL CURB AND GUTTER

* BIKE LANE IS NOT SHOWN



REVERSE SLOPE GUTTER^⑥
(TYPICAL FOR ALL CURB & GUTTER TYPES)

GENERAL NOTES

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

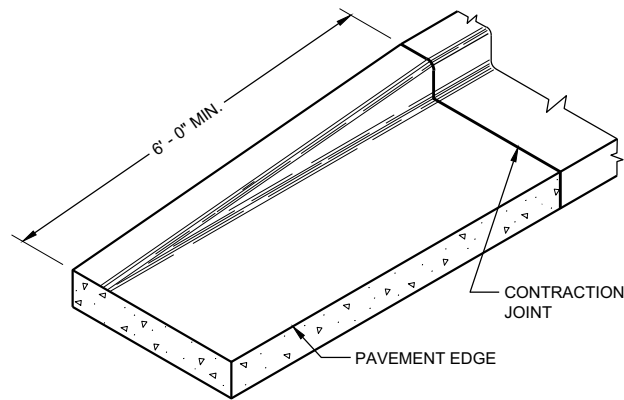
INTEGRAL CURB AND GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB AND GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

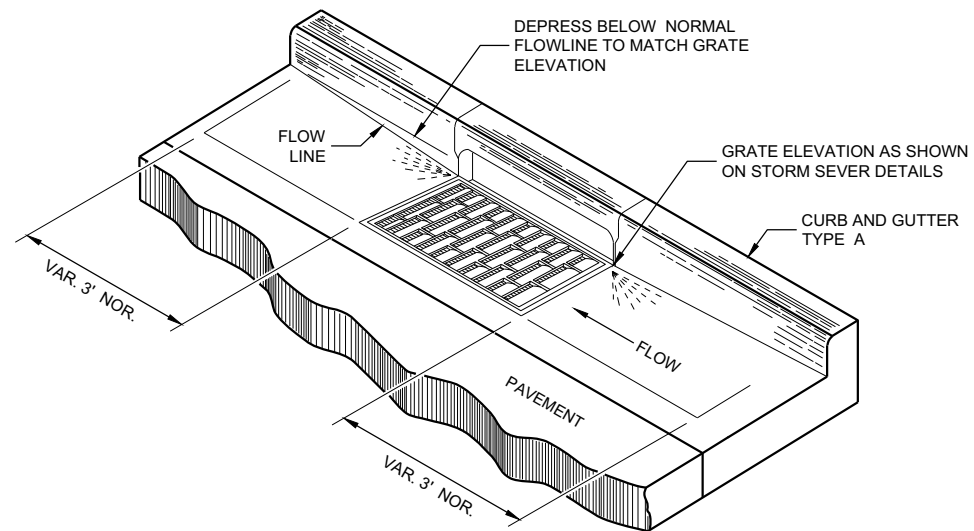
- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ③ USE 8" MINIMUM GUTTER THICKNESS WHEN USED WITH AN ADJACENT CONCRETE TRUCK APRON PLACED BEHIND BACK OF CURB.
- ④ THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑤ UNLESS OTHERWISE NOTED, FOR STAKING PURPOSES THE FACE OF CURB IS 6" FROM THE BACK OF CURB.
- ⑥ WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.
- ⑦ USE 4% GUTTER CROSS SLOPE UNLESS OTHERWISE NOTED IN THE PLANS.
- ⑧ INCLUDE LONGITUDINAL JOINT AND TIE BARS ALONG LANE EDGE WHEN CONCRETE PANEL WIDTH EXCEEDS THE MAXIMUM WIDTH PER TABLE BELOW. LONGITUDINAL JOINT(S) ARE NOT ALLOWED WITHIN TRAFFIC LANES AND BIKE LANES. LONGITUDINAL JOINT MAY BE SAWED.
- ⑨ CONCRETE CURB AND GUTTER 4-INCH SLOPED 30-INCH TYPE "R" AND "T" = 17 INCHES
CONCRETE CURB AND GUTTER 4-INCH SLOPED 36-INCH TYPE "R" AND "T" = 23 INCHES

6

6



END SECTION CURB AND GUTTER



DETAIL OF CURB AND GUTTER AT INLETS
(TYPICAL H INLET COVER SHOWN)

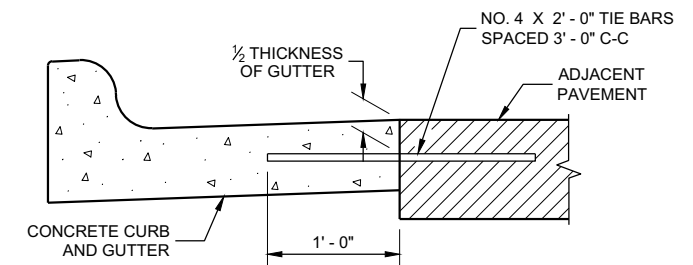
GENERAL NOTES

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

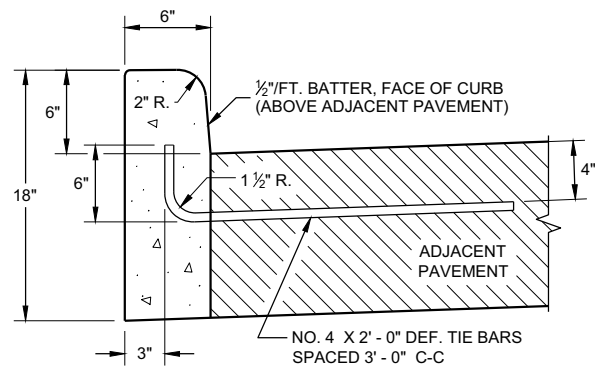
PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2'-0" BEHIND THE BACK OF CURBS.

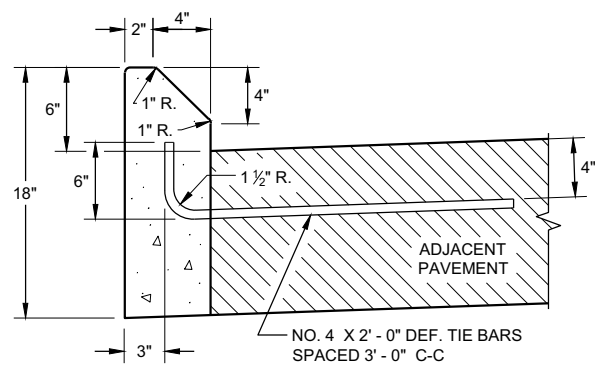
- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑨ REFER TO SDD 08D18 AND 08D19 FOR ADDITIONAL DRIVEWAY ENTRANCE CURB DETAILS.



TYPICAL TIE BAR LOCATION ①

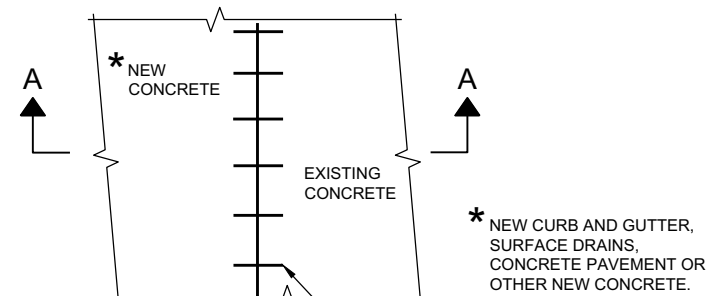


TYPES A ① & D

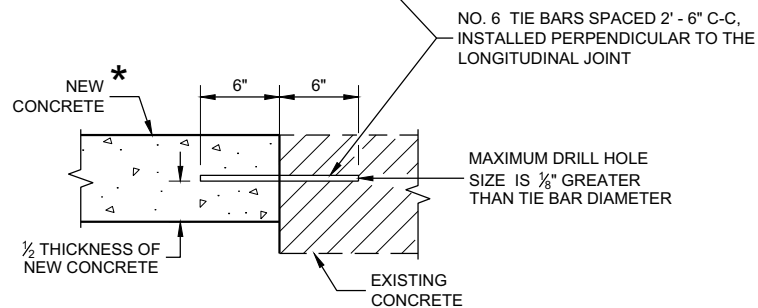


TYPES G ① & J

CONCRETE CURB

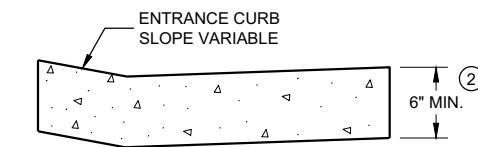


PLAN VIEW



SECTION A - A

TIE BARS DRILLED INTO EXISTING PAVEMENT



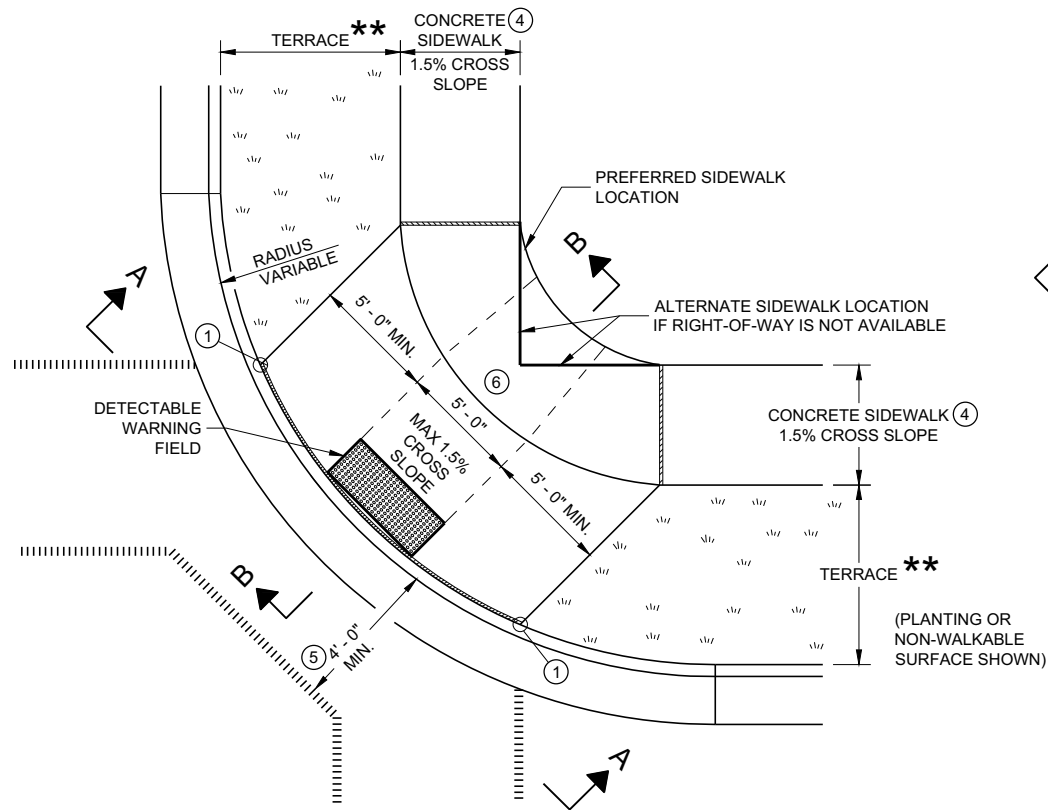
DRIVEWAY ENTRANCE CURB ⑨
(WHEN DIRECTED BY THE ENGINEER)

CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS

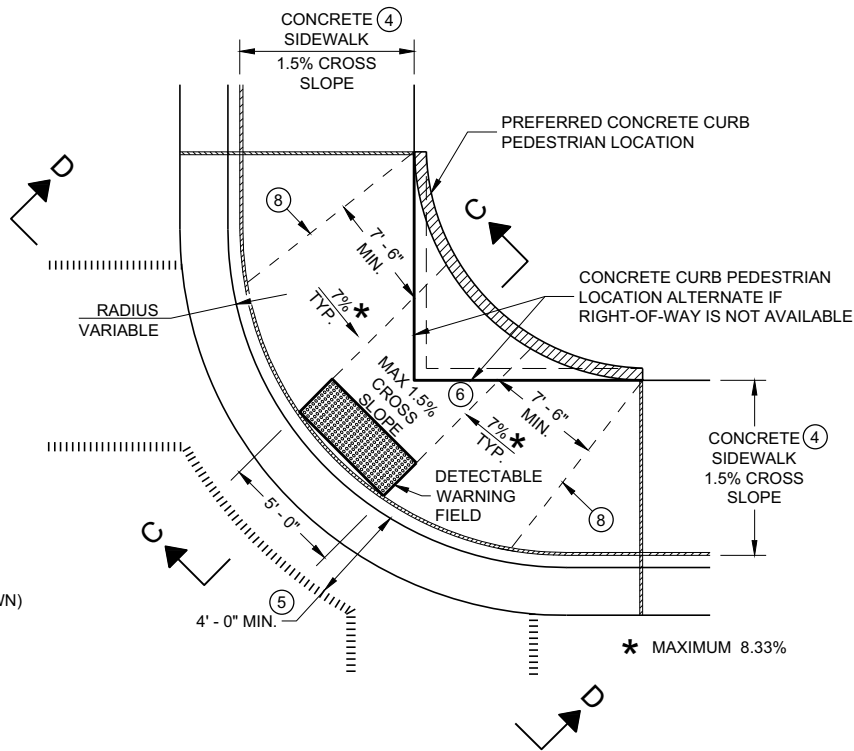
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2021 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT ENGINEER

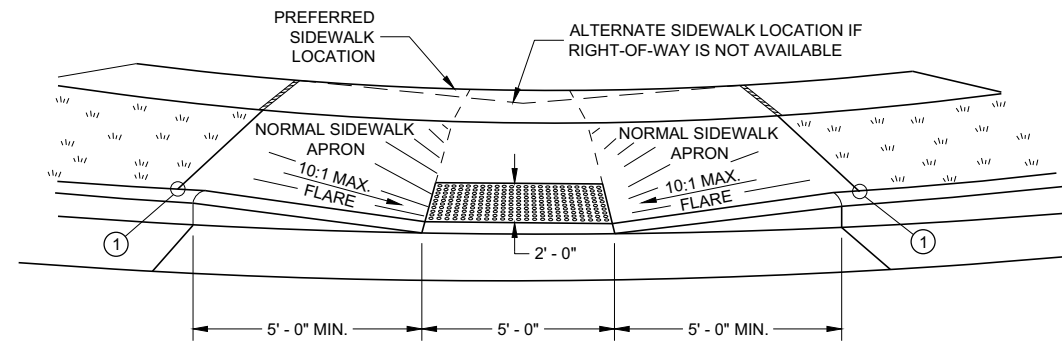
FHWA



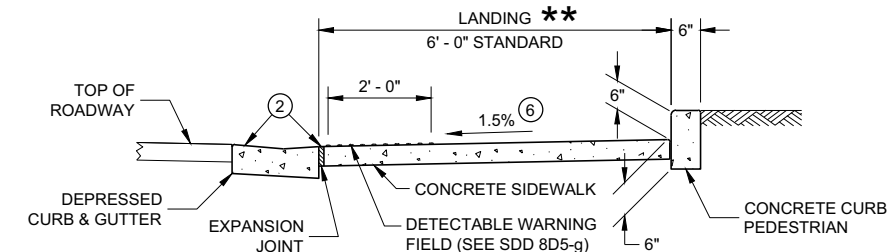
**PLAN VIEW
CURB RAMP TYPE 1
(CENTER OF CORNER RADIUS)**



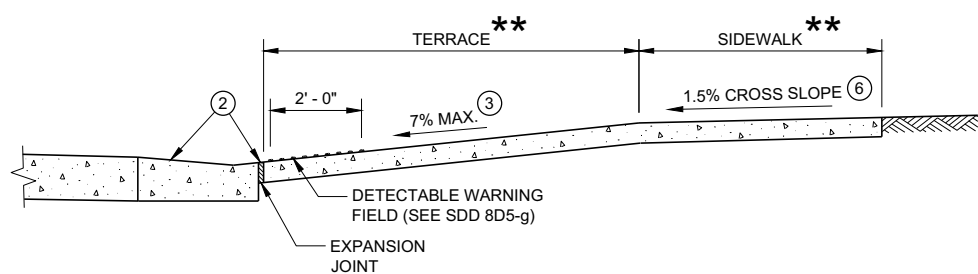
**PLAN VIEW
CURB RAMP TYPE 1 - A
(NO TERRACE)**



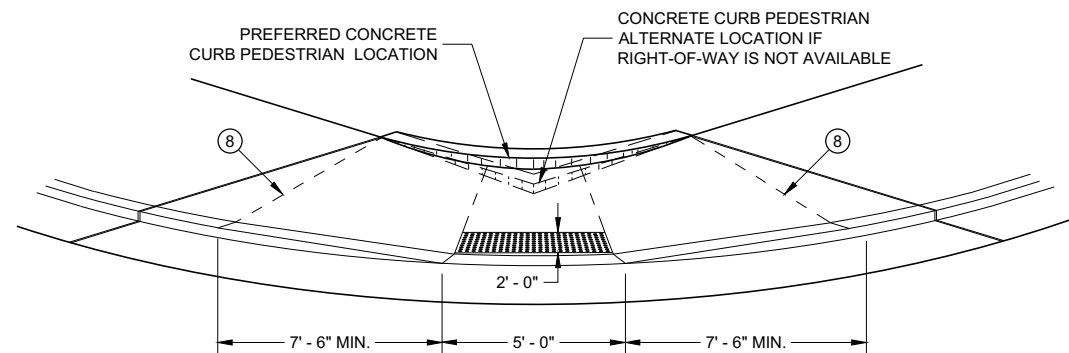
VIEW A - A FOR TYPE 1



SECTION C - C FOR TYPE 1 - A



SECTION B - B FOR TYPE 1



VIEW D - D FOR TYPE 1 - A

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
 DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

WHEN NECESSARY, THE SIDEWALK ELEVATION MAY BE LOWERED TO MEET THE HIGH POINT ON THE RAMP.
 TYPE 1 CURB RAMPS SHALL HAVE A NORMAL SIDEWALK APRON AND CURB ON BOTH SIDES OF RAMP.

DETECTABLE WARNING FIELD SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS "CURB RAMP DETECTABLE WARNING FIELD". THE CONCRETE PEDESTRIAN CURB, IF NEEDED, SHALL BE MEASURED AND PAID BY THE LINEAR FOOT AS "CONCRETE CURB PEDESTRIAN". CONCRETE SIDEWALK IN THE CURB RAMP AREA SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS CONCRETE SIDEWALK, INCLUDING THE AREA UNDER THE DETECTABLE WARNING FIELD.

SELECT CURB RAMP DETECTABLE WARNING FIELD MATERIALS AND DEVICES FROM THE DEPARTMENT'S APPROVED MATERIALS LIST. THE COLOR OF THE DETECTABLE WARNING FIELD IS SPECIFIED ELSEWHERE AND IS INCIDENTAL TO THE BID ITEM OF "CURB RAMP DETECTABLE WARNING FIELD"

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

SURFACE TEXTURE OF THE RAMP SHALL BE OBTAINED BY COARSE BROOMING TRANSVERSE TO THE SLOPE OF THE RAMP.

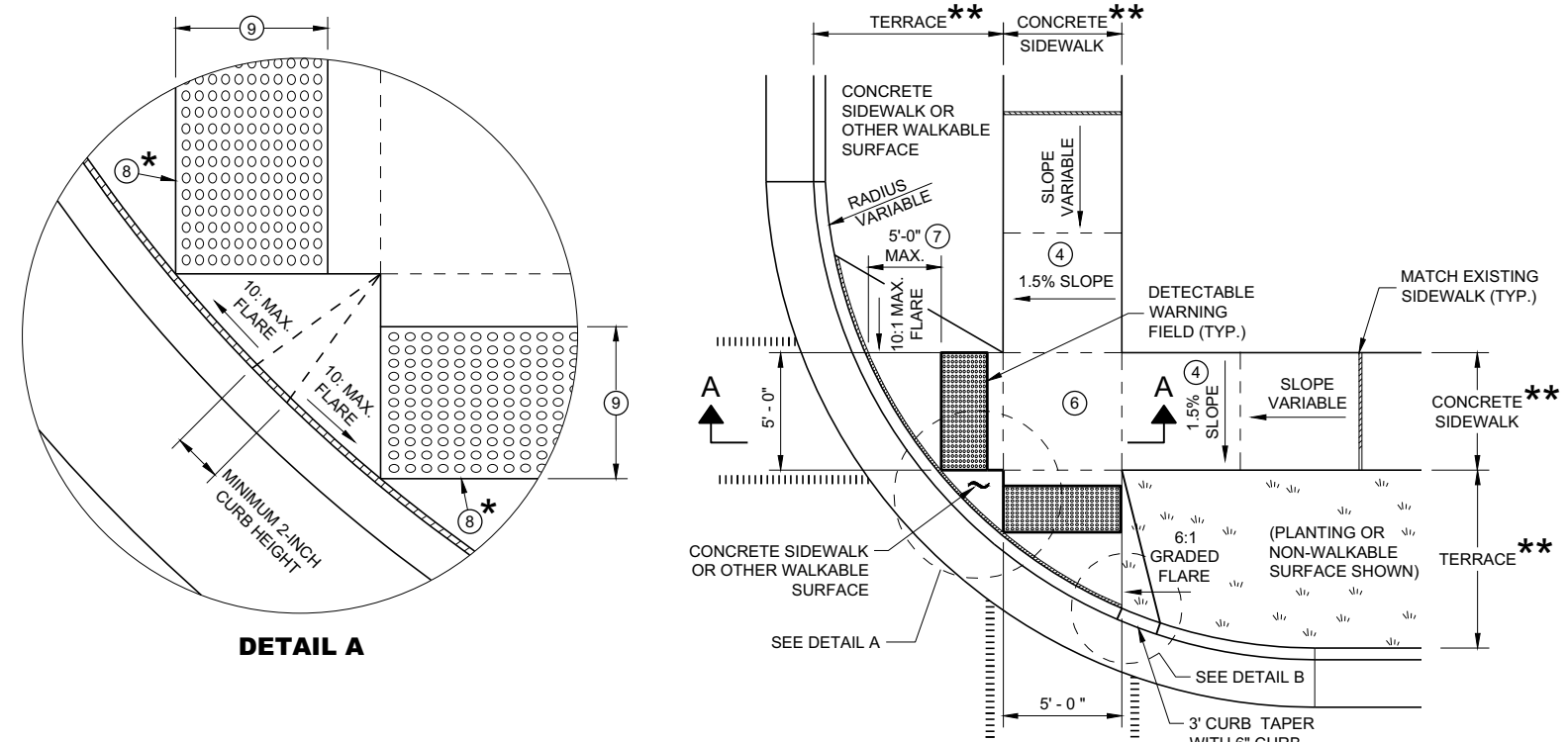
- ① THIS POINT IS AN EXTENSION OF OUTSIDE EDGE OF APPROACHING SIDEWALK WHERE IT MEETS THE BACK OF CONCRETE CURB. POINT LOCATION MAY BE ADJUSTED TO ALIGN WITH BEGINNING OF FULL-HEIGHT CURB IF THIS DISTANCE IS SHORT.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4" INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ MAXIMUM 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑤ PROVIDE A LEVEL LANDING IN THE STREET AND GUTTER AREA (2% MAXIMUM SLOPE IN ANY DIRECTION). WHEN THE GUTTER SLOPE EXCEEDS 2%, CONSTRUCT THE LEVEL LANDING IN THE STREET AREA.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

LEGEND

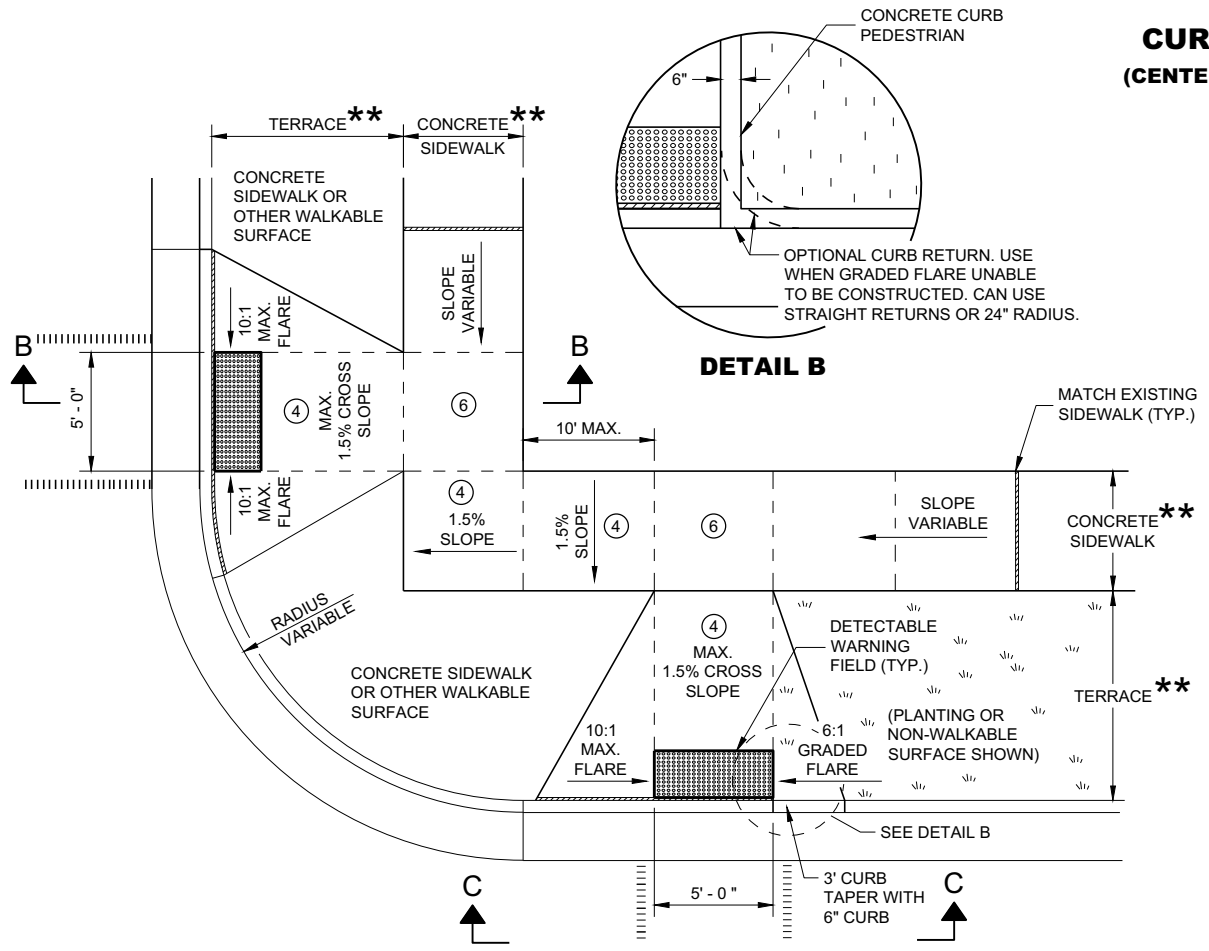
- 1/2" EXPANSION JOINT SIDEWALK
- - - - - CONTRACTION JOINT FIELD LOCATED
- ||||| PAVEMENT MARKING CROSSWALK (WHITE)

**CURB RAMPS
TYPE 1 AND 1-A**

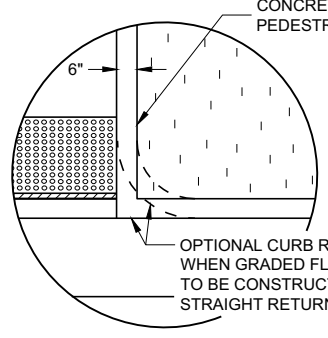
STATE OF WISCONSIN
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PLAN VIEW CURB RAMP TYPE 2 (CENTER OF CORNER RADIUS)



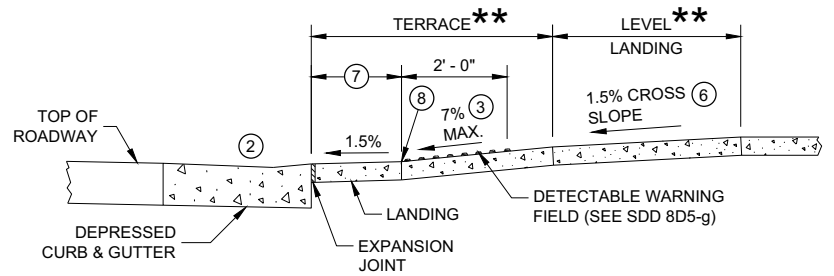
PLAN VIEW CURB RAMP TYPE 3 (OUTSIDE OF CROSSWALK AREA)



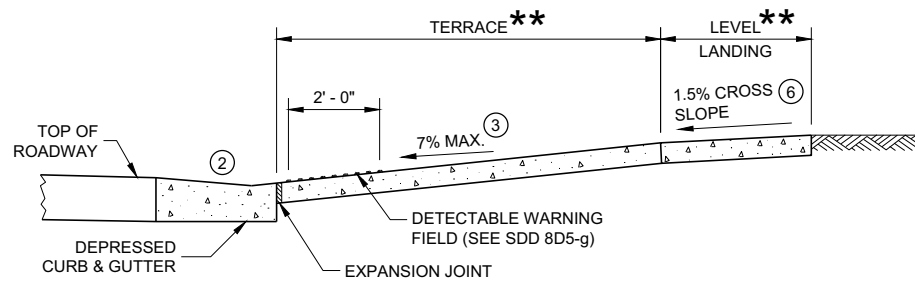
DETAIL B

GENERAL NOTES

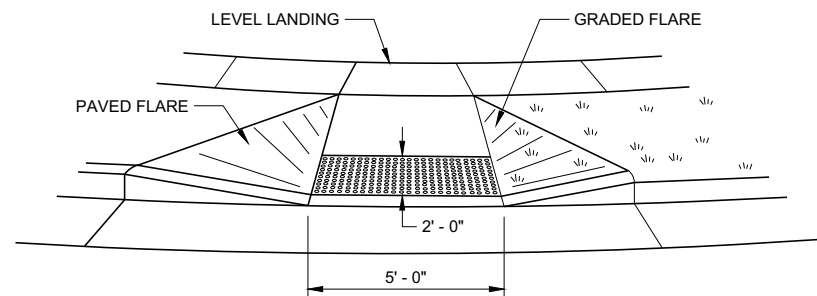
- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE (2.67% OR LESS) AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET X 5 FEET.
- ⑦ WHEN GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑨ WHEN DISTANCE IS LESS THAN 6' - 0", IT MAY BE DIFFICULT TO ACHIEVE A 7% SLOPE OR FLATTER ALONG THE RAMP. REDUCE CURB HEIGHT IN TRIANGLE AREA TO ACHIEVE 7% SLOPE OR FLATTER ON RAMP. CONSTRUCT 2-INCH MINIMUM CURB HEIGHT BETWEEN 10:1 FLARES.



SECTION A - A FOR TYPE 2



SECTION B - B FOR TYPE 3



VIEW C - C FOR TYPE 3

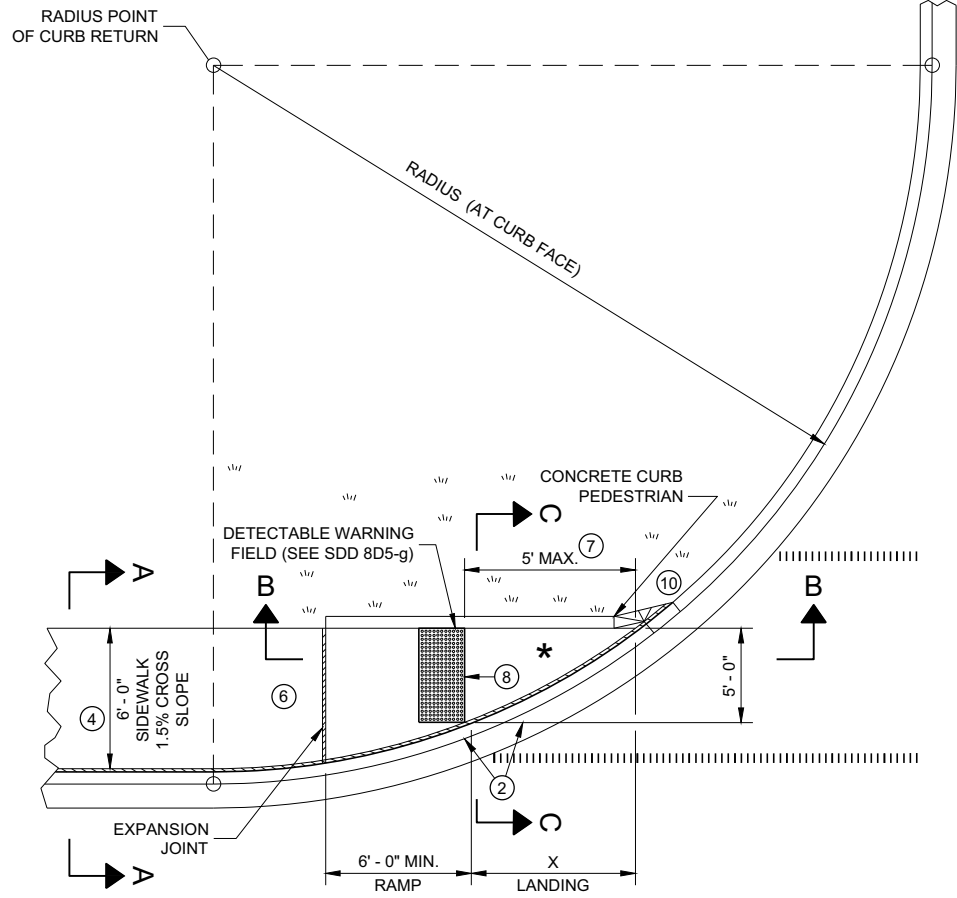
- * MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK
- ** WIDTH SHOWN ELSEWHERE IN THE PLANS

LEGEND

- 1/2" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT SIDEWALK
- PAVEMENT MARKING CROSSWALK (WHITE)

**CURB RAMPS
TYPE 2 AND 3**

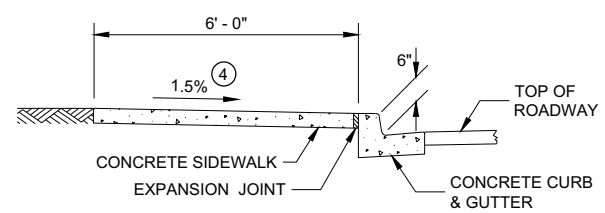
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**PLAN VIEW
CURB RAMP TYPE 4A**

| RADIUS (AT CURB FACE) | X |
|--------------------------|-------------|
| 10 FEET | 4' - 7" |
| 15 FEET | 6' - 5 1/2" |

INTERMEDIATE RADII CAN BE INTERPOLATED



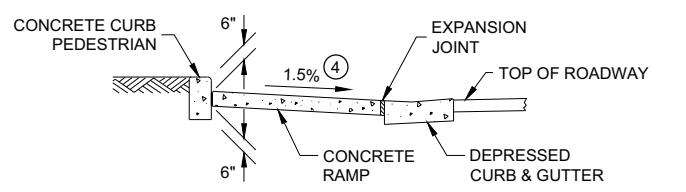
SECTION A - A FOR TYPE 4A

GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4" INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑦ WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑩ INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

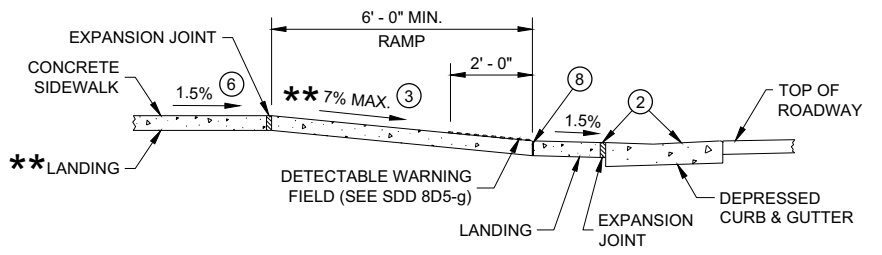
LEGEND

- 1/2" EXPANSION JOINT SIDEWALK
- - - CONTRACTION JOINT SIDEWALK
- ||||| PAVEMENT MARKING CROSSWALK (WHITE)



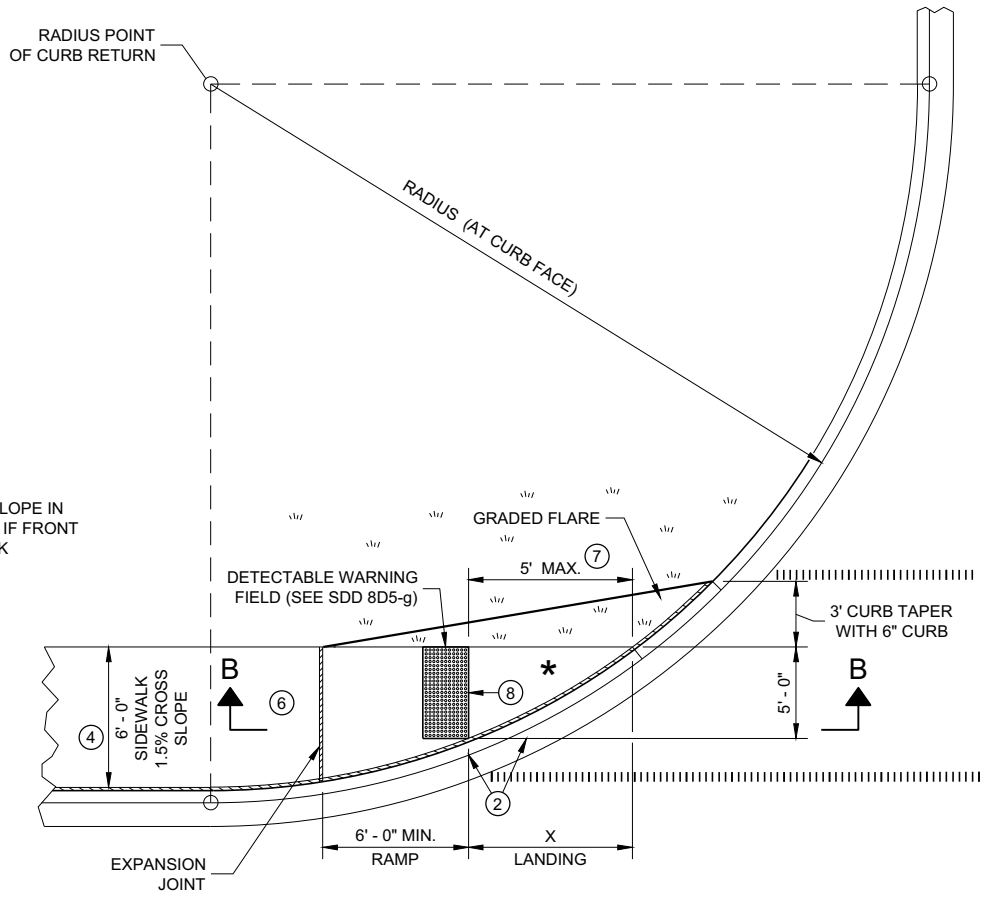
SECTION C - C FOR TYPE 4A

* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IF FRONT OF GRADE BREAK

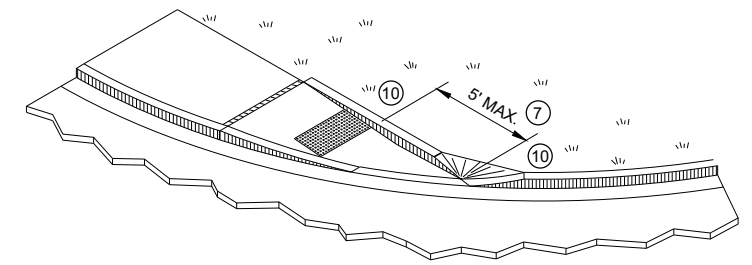


**SECTION B - B FOR
TYPE 4A AND TYPE 4A1**

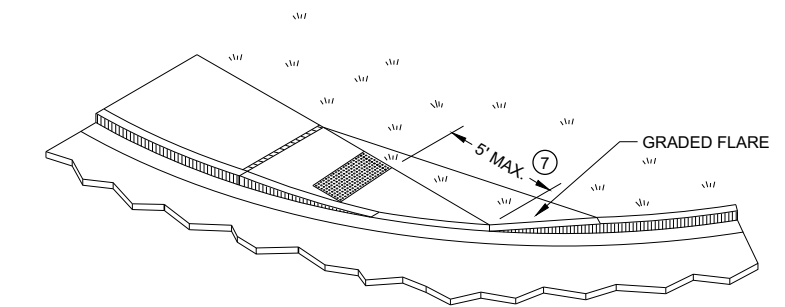
** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED



**PLAN VIEW
CURB RAMP TYPE 4A1**



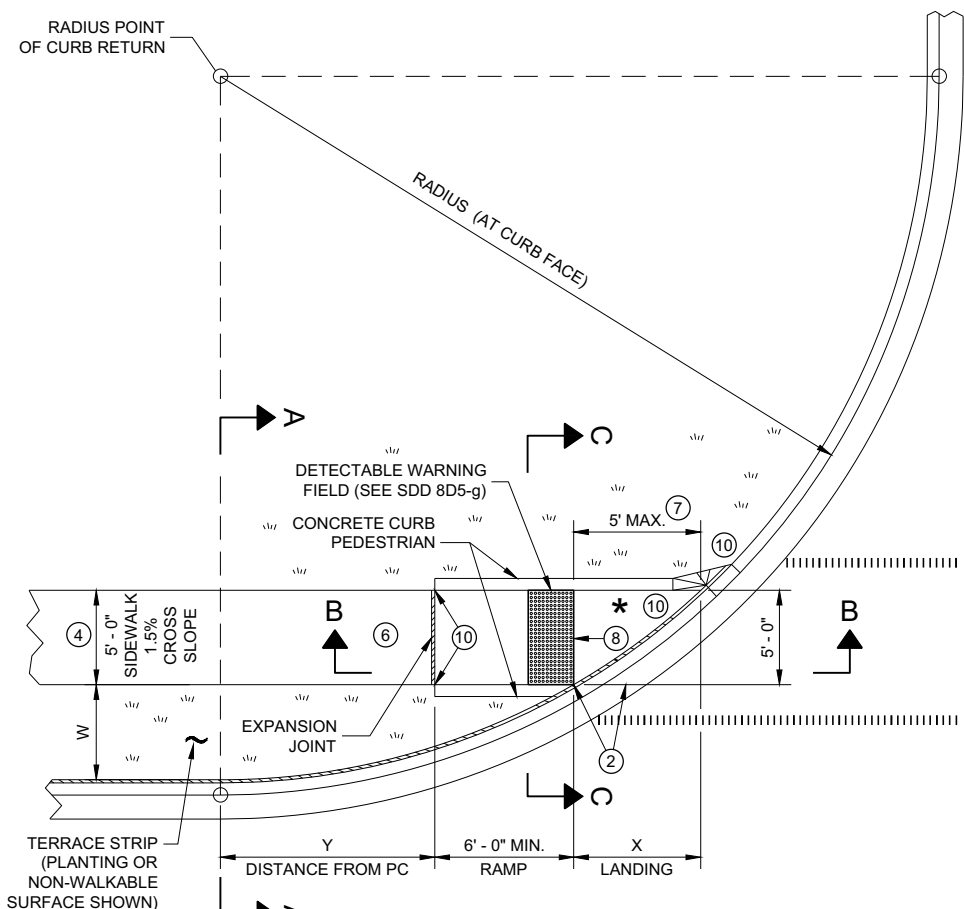
ISOMETRIC VIEW FOR TYPE 4A



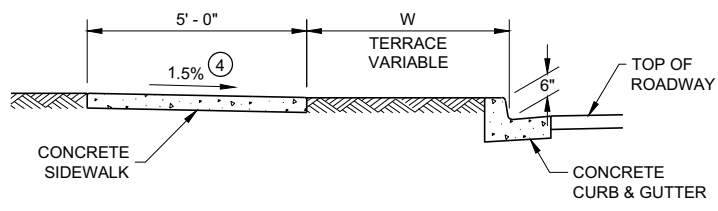
ISOMETRIC VIEW FOR TYPE 4A1

**CURB RAMPS
TYPE 4A AND 4A1**

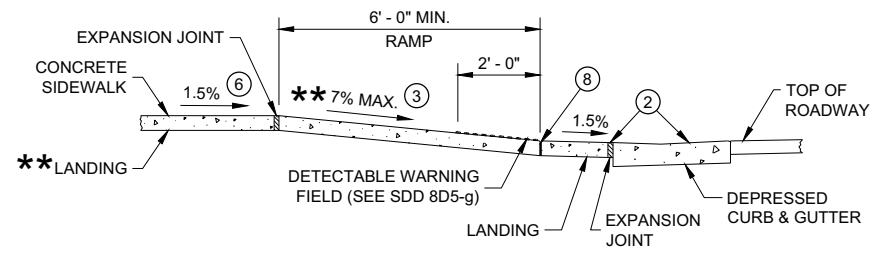
STATE OF WISCONSIN
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**PLAN VIEW
CURB RAMP TYPE 4B**



SECTION A - A FOR TYPE 4B



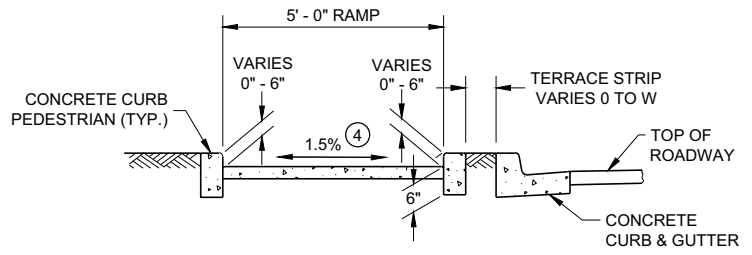
**SECTION B - B FOR
TYPE 4B AND TYPE 4B1**

** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

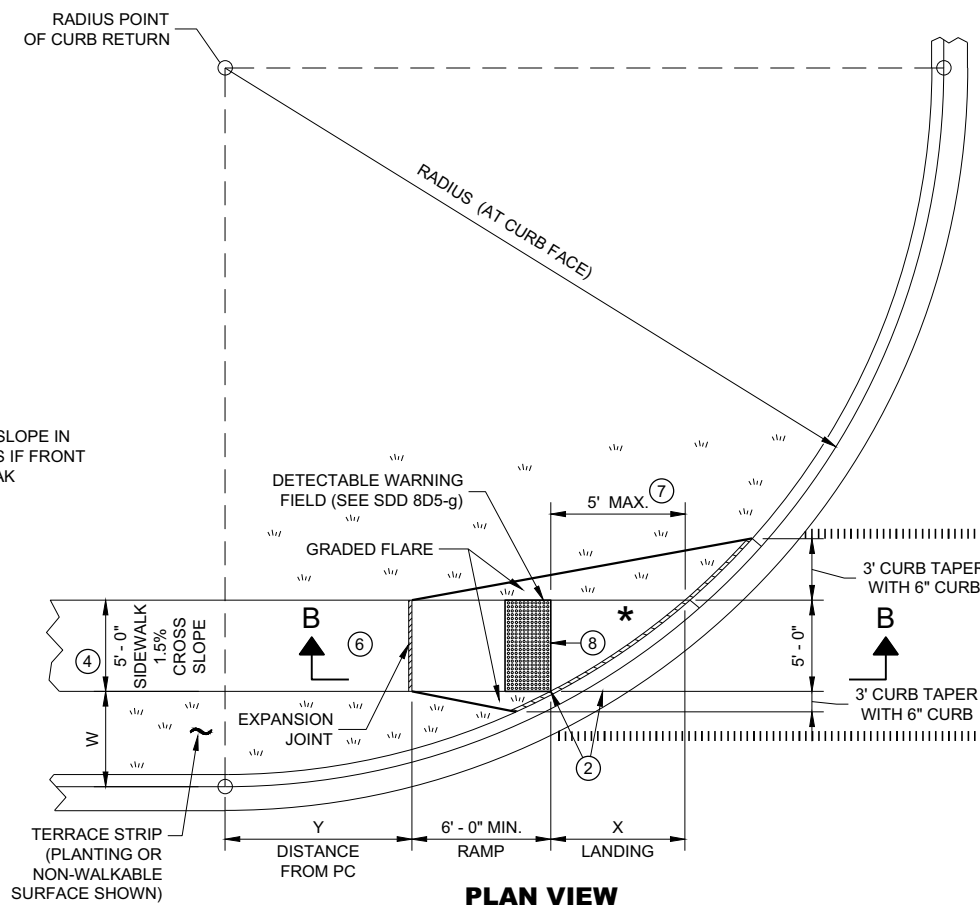
* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IF FRONT OF GRADE BREAK

| RADIUS (AT CURB FACE) | W = 3' - 0" | | W = 4' - 0" | | W = 5' - 0" | | W = 6' - 0" | | W = 7' - 0" | | W = 8' - 0" | | W = 9' - 0" | | W = 10' - 0" | |
|-----------------------|--------------|-------------|--------------|--------------|-------------|-------------|-------------|--------------|--------------|--------------|-------------|--------------|-------------|--------------|--------------|--------------|
| | X | Y | X | Y | X | Y | X | Y | X | Y | X | Y | X | Y | X | Y |
| 10 FEET | 2' - 10 1/4" | 0' - 5" | 2' - 1" | 1' - 4 1/2" | 1' - 5" | 2' - 1" | 0' - 10" | 2' - 7 1/2" | 0' - 3 1/4" | 3' - 0 1/4" | | | | | | |
| 15 FEET | 4' - 6 3/4" | 2' - 1 3/4" | 3' - 9" | 3' - 5 3/4" | 3' - 1 1/4" | 4' - 6" | 2' - 6 3/4" | 5' - 4 1/2" | 2' - 1" | 6' - 1" | 1' - 8" | 6' - 8 1/2" | 1' - 3 1/4" | 7' - 2 1/2" | 0' - 10 3/4" | 7' - 7 1/4" |
| 20 FEET | 5' - 9 3/4" | 3' - 6 1/2" | 4' - 11 1/2" | 5' - 1 3/4" | 4' - 3 1/4" | 6' - 5 1/2" | 3' - 8 3/4" | 7' - 7" | 3' - 3" | 8' - 6 1/2" | 2' - 10" | 9' - 4 1/2" | 2' - 5 1/2" | 10' - 1 1/4" | 2' - 1 1/4" | 10' - 9" |
| 30 FEET | | | 6' - 9 1/4" | 7' - 11 1/4" | 6' - 0 1/4" | 9' - 8" | 5' - 5" | 11' - 1 3/4" | 4' - 10 3/4" | 12' - 5 3/4" | 4' - 5 1/2" | 13' - 7 3/4" | 4' - 0 3/4" | 14' - 8 1/2" | 3' - 8 1/2" | 15' - 8 1/4" |
| 40 FEET | | | | | | | | | 6' - 1 3/4" | 15' - 8 1/2" | 5' - 8" | 17' - 2" | 5' - 3" | 18' - 5 3/4" | 4' - 10 3/4" | 19' - 8 1/4" |
| 50 FEET | | | | | | | | | | | | | | | 5' - 10 1/4" | 23' - 2" |

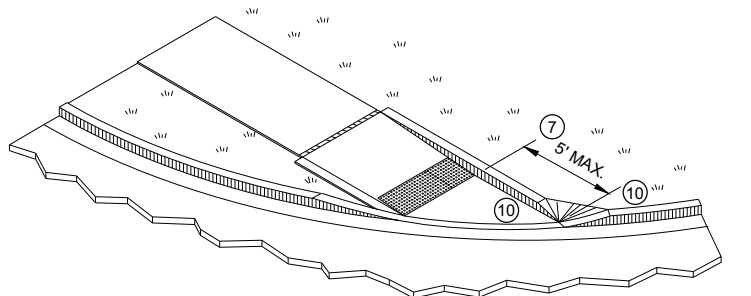
INTERMEDIATE RADII CAN BE INTERPOLATED
DIMENSION "Y" IS CALCULATED BASED ON 6'-0" RAMP LENGTH
DIMENSION "X" IS CALCULATED BASED ON 5'-0" SIDEWALK WIDTH



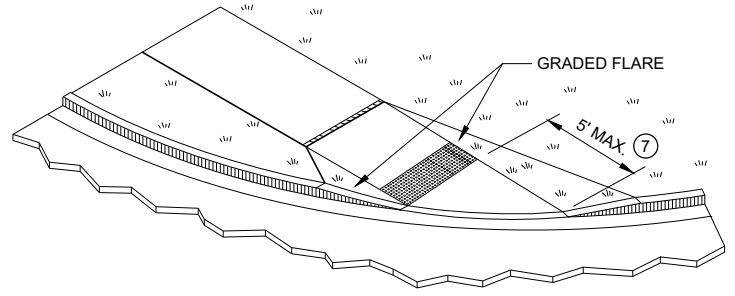
SECTION C - C FOR TYPE 4B



**PLAN VIEW
CURB RAMP TYPE 4B1**



ISOMETRIC VIEW FOR TYPE 4B



ISOMETRIC VIEW FOR TYPE 4B1

LEGEND

- 1/2" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT SIDEWALK
- PAVEMENT MARKING CROSSWALK (WHITE)

GENERAL NOTES

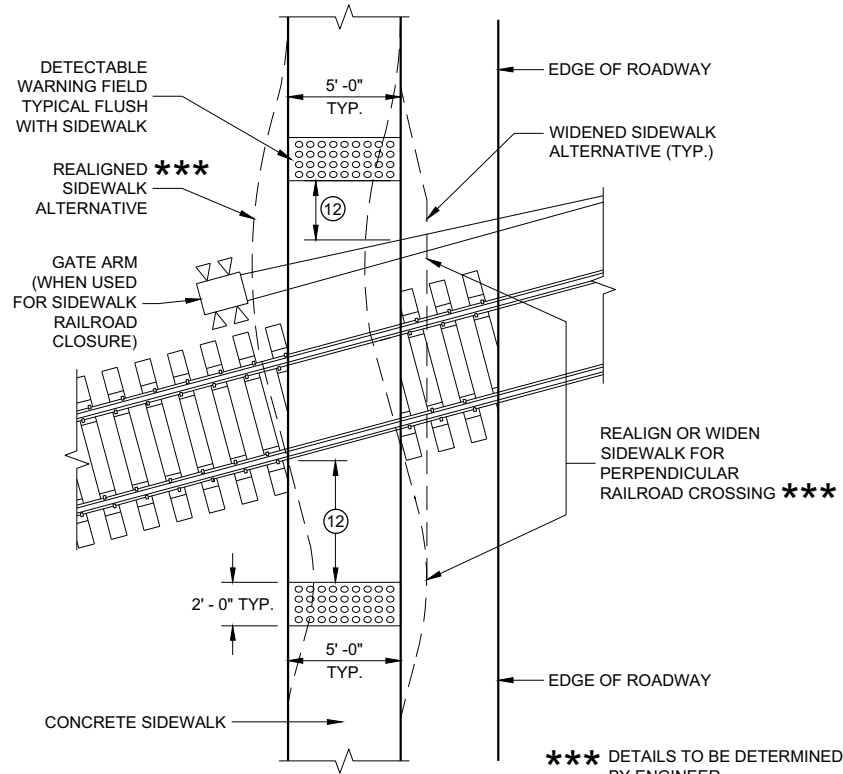
- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- 2 GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/2 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- 3 AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- 4 ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- 6 PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- 7 WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- 8 PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- 10 INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

SDD 08D05 - 20d

SDD 08D05 - 20d

**CURB RAMPS
TYPE 4B AND 4B1**

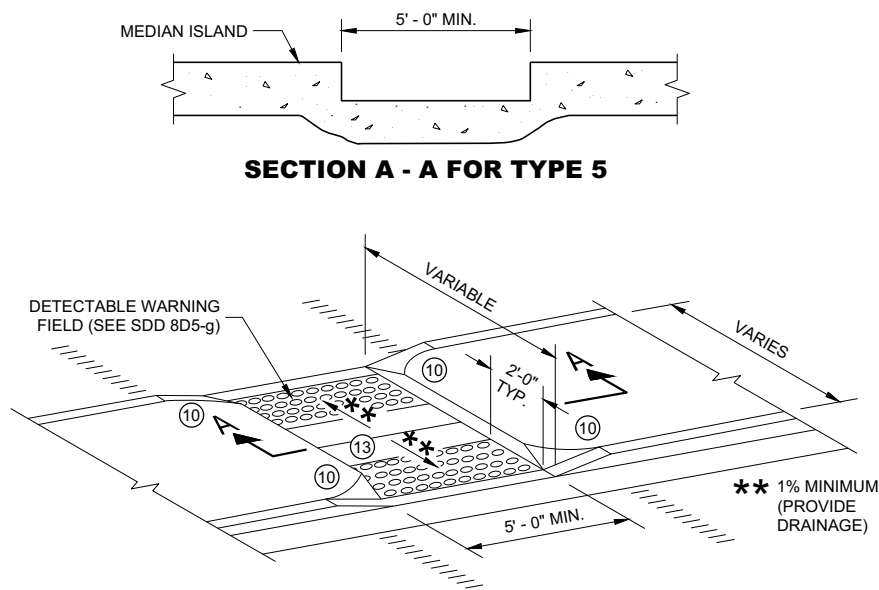
STATE OF WISCONSIN
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CURB RAMP TYPE 8

DETECTABLE WARNINGS AT RAILROAD CROSSING

*** DETAILS TO BE DETERMINED BY ENGINEER



CURB RAMP TYPE 5
MEDIAN ISLAND
NON-ELEVATED PEDESTRIAN CROSSING

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

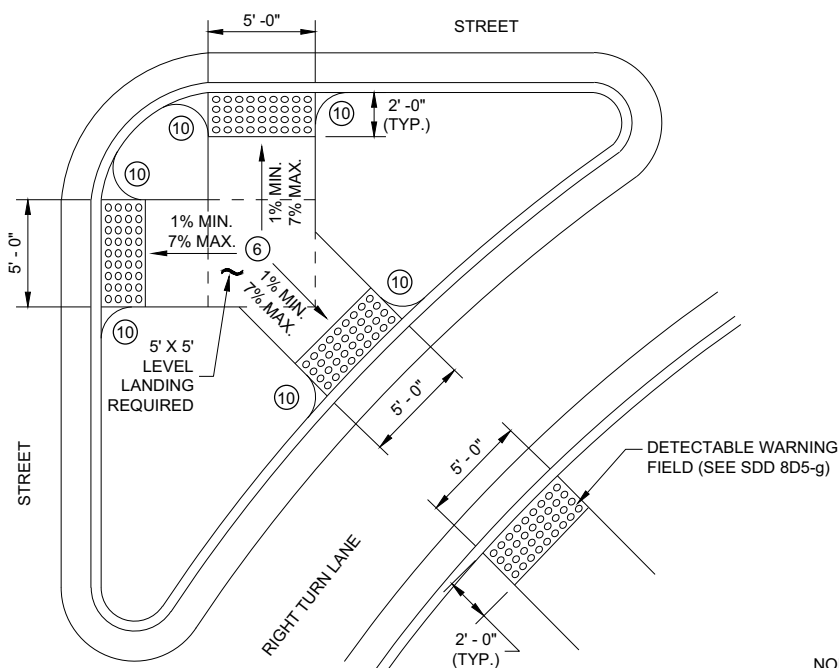
SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑩ INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.
- ⑪ SLOPE SIDEWALK TOWARD LANDING AS SHOWN WHERE THERE IS NO TERRACE OR WHERE THE TERRACE WIDTH IS LESS THAN 6 FEET WIDE.
- ⑫ THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO A RAILROAD CROSSING SHALL BE 1.5 FEET ±0.1' FROM THE FACE OF THE GATE ARM IF THE GATE ARM EXTENDS ACROSS THE SIDEWALK. WHERE THERE IS NO PEDESTRIAN GATE, THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO THE RAILROAD CROSSING SHALL BE 15 FEET FROM THE NEAREST RAIL.
- ⑬ DO NOT INSTALL DETECTABLE WARNING FIELDS AT THE EDGES OF STREET-LEVEL PEDESTRIAN REFUGE ISLANDS IF A MINIMUM 2 FOOT CONCRETE SURFACE WITHOUT DETECTABLE WARNINGS (MEASURED IN THE DIRECTION OF PEDESTRIAN TRAVEL) CANNOT BE ACHIEVED.

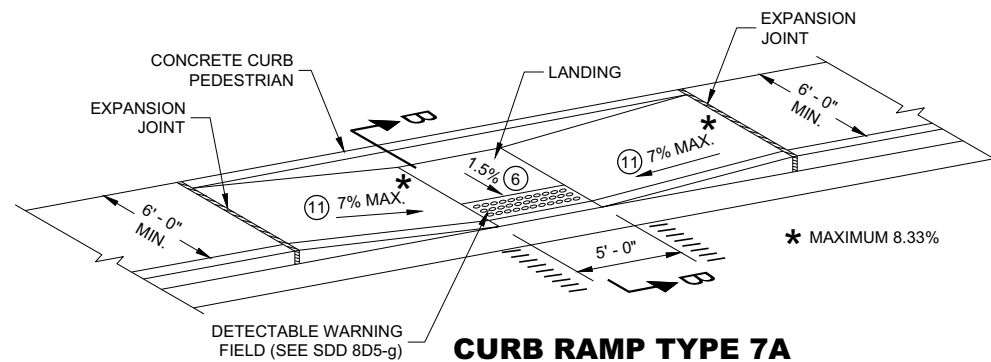
LEGEND

- ===== 1/2" EXPANSION JOINT SIDEWALK
- - - - - CONTRACTION JOINT FIELD LOCATED
- ||||||| PAVEMENT MARKING CROSSWALK (WHITE)

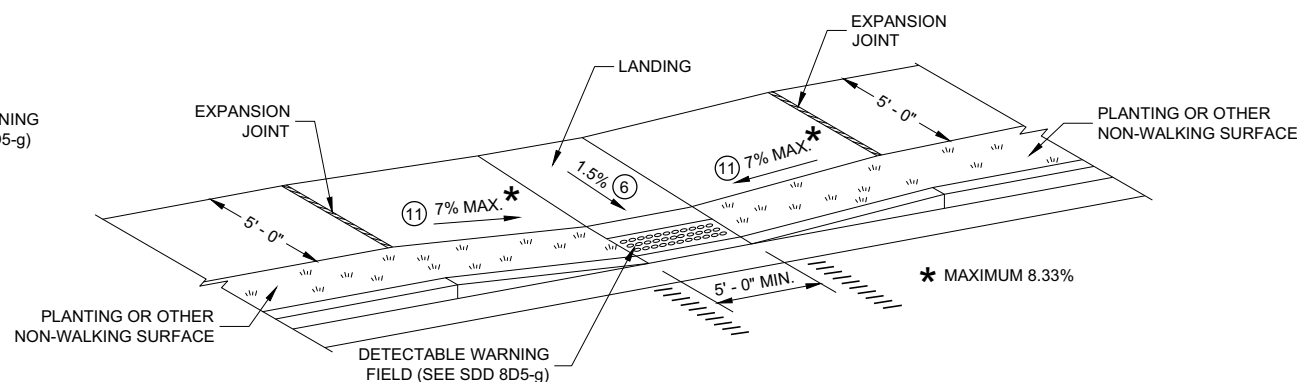


CURB RAMP TYPE 6
DETECTABLE WARNING AT ISLANDS

REFER TO GENERAL NOTES ② AND ③ FOR ALL ISLAND CURB RAMPS

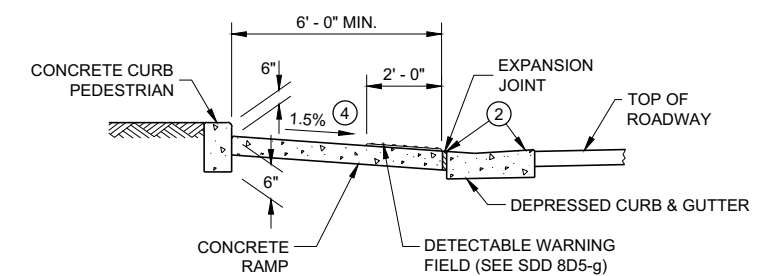


CURB RAMP TYPE 7A
MID BLOCK CROSSING



CURB RAMP TYPE 7B
MID BLOCK CROSSING

NOTE: THESE PARALLEL AND PARALLEL/PERPENDICULAR CURB RAMPS MAY BE USED AT INTERSECTIONS AND MID BLOCK LOCATIONS.

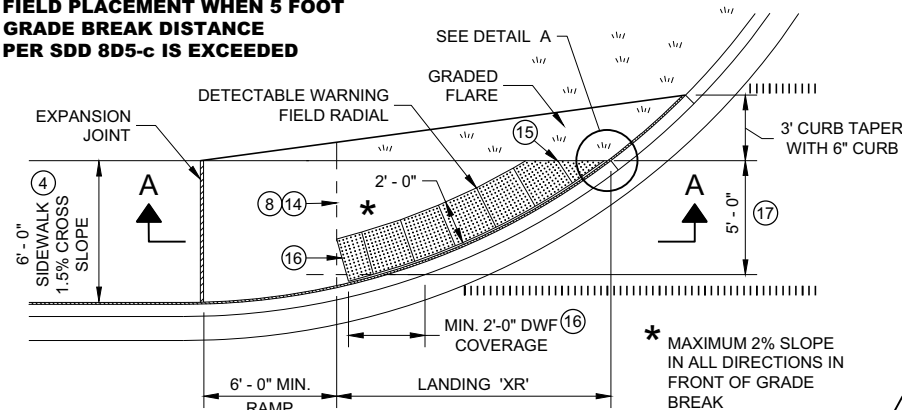


SECTION B - B FOR TYPE 7A

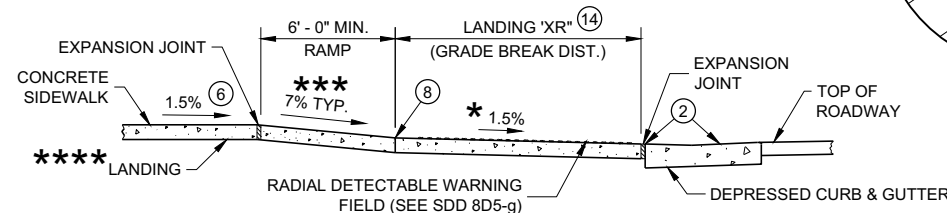
CURB RAMPS
TYPE 5, 6, 7A, 7B & 8

STATE OF WISCONSIN
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RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-c IS EXCEEDED



**PLAN VIEW
CURB RAMP TYPE 4A1
(GRADE BREAK DISTANCE GREATER THAN 5 FEET)**

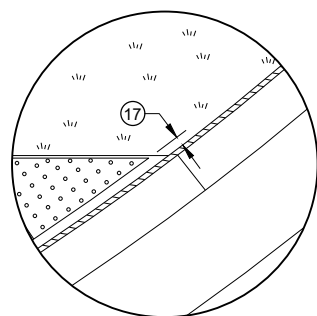


SECTION A - A FOR TYPE 4A1

**** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

*** MAXIMUM 8.33%

- LEGEND**
- 1/2" EXPANSION JOINT SIDEWALK
 - - - - - CONTRACTION JOINT SIDEWALK
 - ||||| PAVEMENT MARKING CROSSWALK (WHITE)

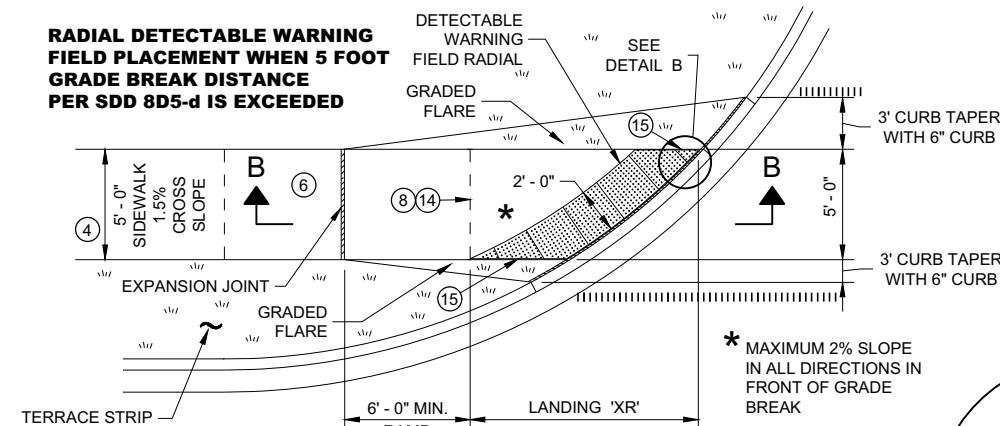


DETAIL A

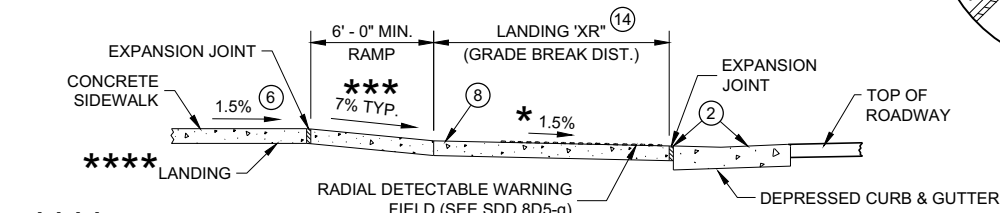
GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- APPLY RADIAL DETECTABLE WARNING PLACEMENT SIMILARLY FOR TYPE 4A AND 4A1 CURB RAMPS AND SIMILARLY FOR TYPE 4B AND 4B1 CURB RAMPS. TYPE 4A AND 4B RAMPS ARE NOT SHOWN.
- REFER TO SDD 8D5-g FOR ADDITIONAL RADIAL PLATE REQUIREMENTS.
- FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FILED ARE PROHIBITED.
- DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.
- 2 GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- 3 AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- 4 ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- 6 PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET BY 5 FEET.
- 8 PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- 14 CONSULT ENGINEER IF GRADE BREAK LOCATION (END OF LANDING DIMENSION "XR") REQUIRES FIELD ADJUSTMENT WHEN ESTABLISHING FINAL RADIAL DETECTABLE WARNING FIELD LOCATION.
- 15 FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN 1/8" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.
- 16 USE 1' X 2" RECTANGULAR END PLATE AT END OF TYPE 4A1 RAMP AND PROVIDE MINIMUM 2' - 0" DETECTABLE WARNING FIELD COVERAGE (IN DIRECTION OF PEDESTRIAN TRAVEL) ALONG THE ENTIRE CURB RAMP WIDTH.
- 17 A MAXIMUM 3 INCH CONCRETE BORDER WITH IS ALLOWABLE IN FROM OF RADIAL DETECTABLE WARNING FIELD FOR CONSTRUCTABILITY PURPOSES. CONCRETE BORDER WIDTH MAY VARY UP TO 1 INCH.

RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-d IS EXCEEDED



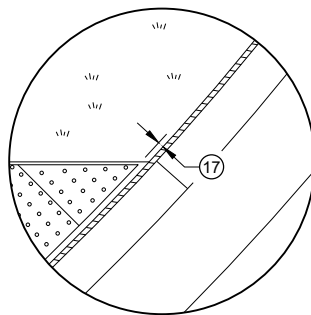
**PLAN VIEW
CURB RAMP TYPE 4B1
(GRADE BREAK DISTANCE GREATER THAN 5 FEET)**



SECTION B - B FOR TYPE 4B1

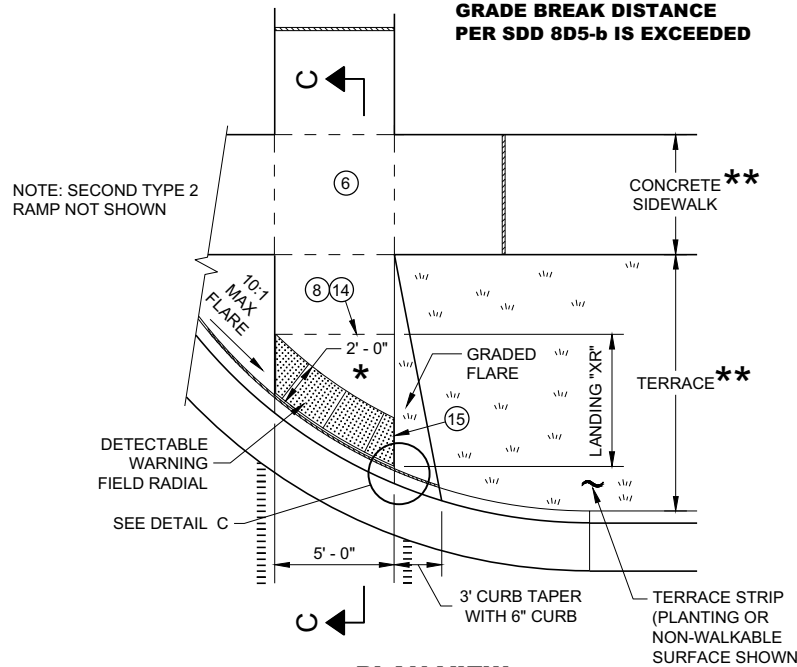
**** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

*** MAXIMUM 8.33%



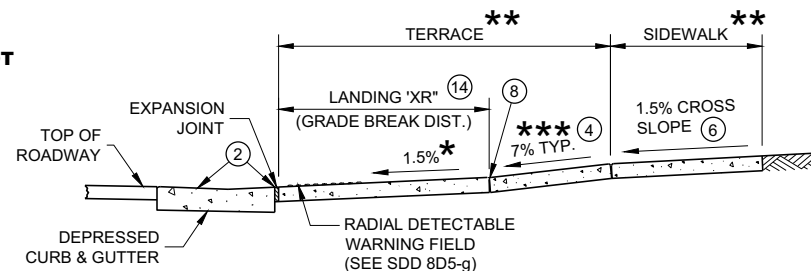
DETAIL B

RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-b IS EXCEEDED



**PLAN VIEW
CURB RAMP TYPE 2
(GRADE BREAK DISTANCE GREATER THAN 5 FEET)
(ON LINE WITH SIDEWALK)**

NOTE: SECOND TYPE 2 RAMP NOT SHOWN

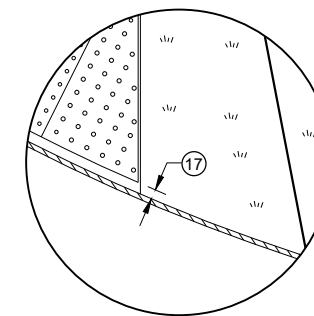


SECTION C - C FOR TYPE 2

* MAXIMUM 2% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK

** WIDTH SHOWN ELSEWHERE IN THE PLANS

*** MAXIMUM 8.33%



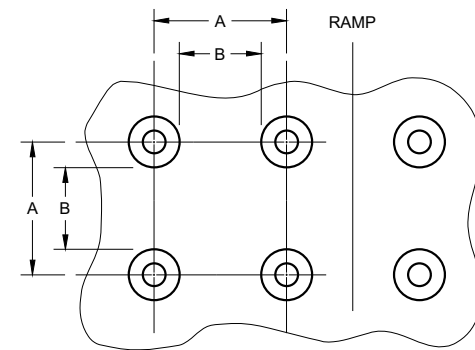
DETAIL C

**CURB RAMPS
RADIAL DETECTABLE WARNING
FIELD APPLICATIONS**

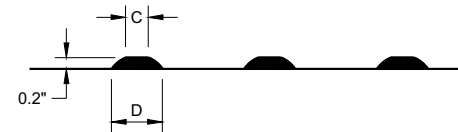
STATE OF WISCONSIN
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| | MIN. | MAX. |
|---|-------|------|
| A | 1.6" | 2.4" |
| B | 0.65" | 1.5" |
| C | * | * |
| D | 0.9" | 1.4" |

* THE C DIMENSION IS 50% TO 65% OF THE D DIMENSION.

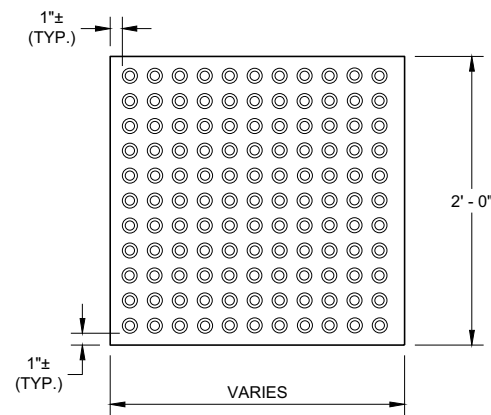


PLAN VIEW

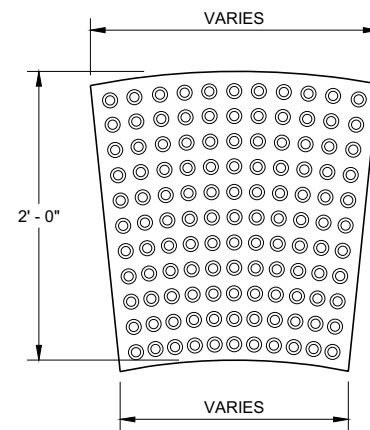


ELEVATION VIEW

**TRUNCATED DOMES
DETECTABLE WARNING PATTERN DETAIL**

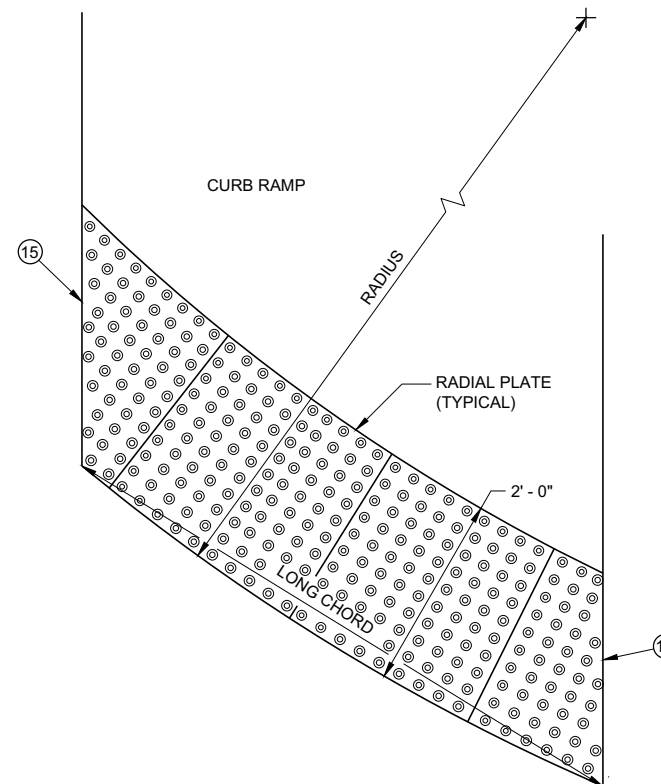


**RECTANGULAR
PLATES**

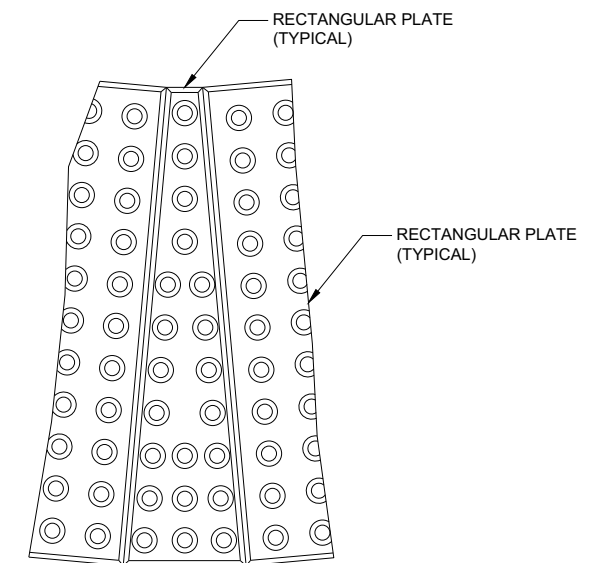


**RADIAL
PLATES**

**PLAN VIEW
DETECTABLE WARNING FIELDS (TYPICAL)**



**PLAN VIEW
RADIAL DETECTABLE
WARNING FIELD ATTRIBUTES**



**PLAN VIEW
RADIAL WEDGE PLATE
CONNECTION DETAIL**

GENERAL NOTES

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AT A CURB RAMP SHALL BE FROM THE SAME MANUFACTURER.

PLACE ALL DETECTABLE WARNING FIELD SYSTEMS IN ACCORDANCE TO THE MANUFACTURER'S RECOMMENDATION.

FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FIELD ARE PROHIBITED.

DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.

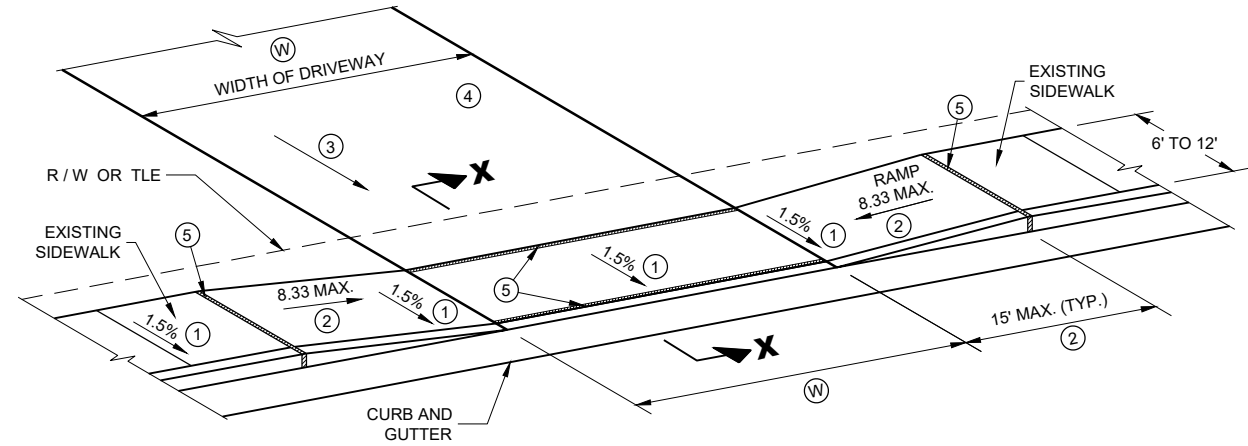
FOR RADIAL DETECTABLE WARNING FIELD APPLICATIONS WHERE STANDARD RADIAL PLATES ARE NOT AVAILABLE AT AN INTERSECTION CURB RADIUS, A COMBINATION OF SQUARE OR RECTANGULAR PLATES AND RADIAL PLATES MAY BE USED TO FORM RADIAL CONFIGURATION. RADIAL WEDGE PLATES IN COMBINATION WITH SQUARE PLATES ARE ALSO ACCEPTABLE. FOLLOW MANUFACTURER'S RECOMMENDATIONS.

REFER TO CONTRACT AND STANDARD SPECIFICATIONS FOR FIELD CUTTING REQUIREMENTS.

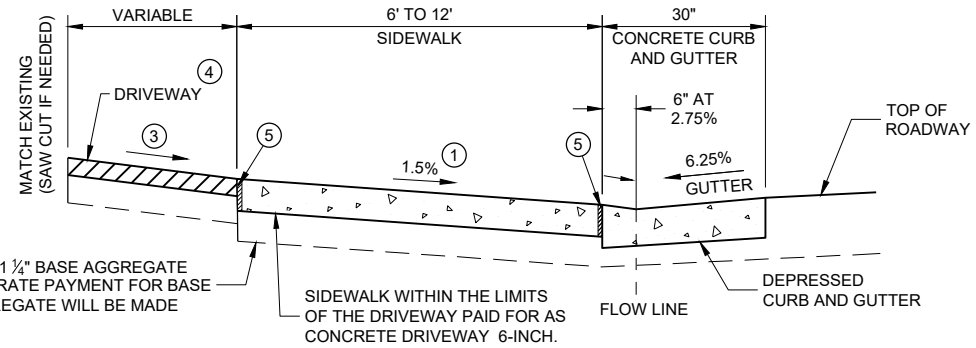
DO NOT EMBED IN CONCRETE ANY FIELD-CUT PLATES WITH CUT EDGES SHORTER THAN 6 INCHES. CONSULT WITH MANUFACTURER FOR RE-DRILLING AND ANCHORING REQUIREMENTS OF FIELD-CUT PLATES.

15 FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN 1/8" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.

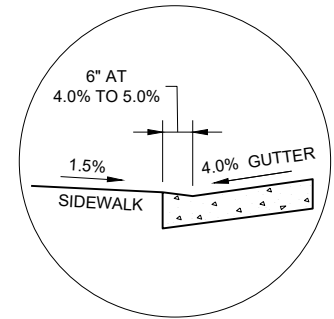
| CURB RAMPS RECTANGULAR AND RADIAL DETECTABLE WARNING PLATES | |
|--|---|
| STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION | |
| APPROVED May 2019 DATE | /S/ Rodney Taylor ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR |
| FHWA | |



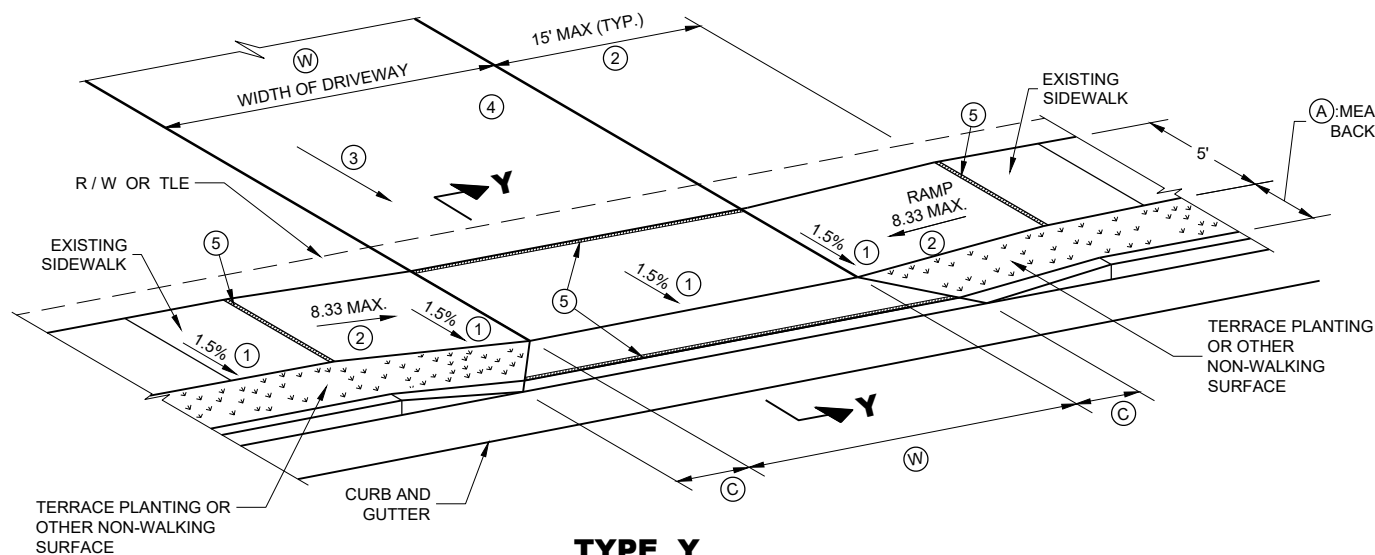
TYPE X
SIDWALK ABUTS CURB AND GUTTER
TERRACE VARIES 0 TO 3 FEET



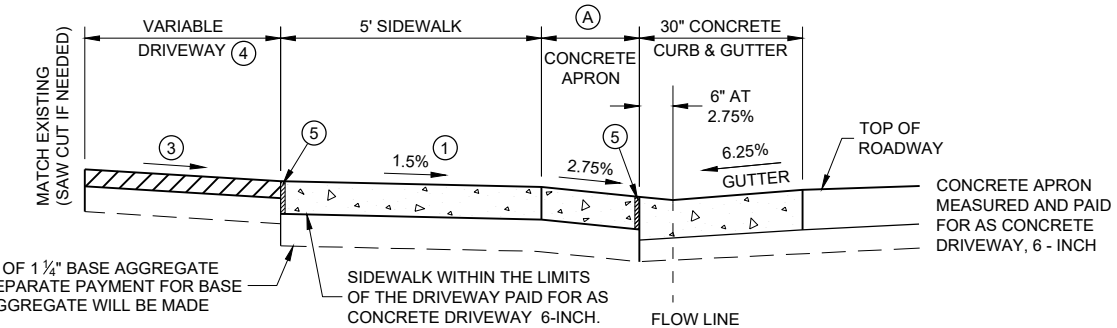
SECTION X - X



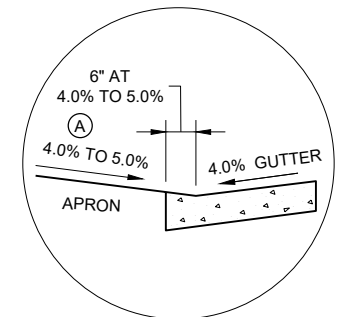
SECTION X - X
4% GUTTER SLOPE



TYPE Y
SIDWALK WITH NARROWER TERRACE
TERRACE VARIES 4 TO 6 FEET



SECTION Y - Y
DRIVEWAY DETAIL WITH CONCRETE
CURB AND GUTTER
(URBAN AND SUBURBAN)



SECTION Y - Y
4% GUTTER SLOPE

(W): 12' MIN. - 24' MAX. RESIDENTIAL AND NON-COMMERCIAL (PE & FE)
 16' MIN. - 35' MAX. COMMERCIAL (CE)

TABLE Y

| (A) FEET | (C) FEET |
|----------|----------|
| 3.5' | 2.0' |
| 4.5' | 3.0' |
| 5.5' | 3.5' |

(A): MEASURE FROM BACK OF CURB

6" OF 1 1/4" BASE AGGREGATE SEPARATE PAYMENT FOR BASE AGGREGATE WILL BE MADE

NOTE: SIDEWALK MAY BE DEPRESSED IN DRIVEWAY AREAS

GENERAL NOTES

PROVIDE CONSTRUCTION JOINTS ALONG THE CENTER OF THE CONCRETE FOR DRIVEWAYS UNDER 20 FEET IN WIDTH AND AT THE THIRD POINTS OVER 20 FEET IN WIDTH.

(W) IS SHOWN ON PLAN AND PROFILE SHEETS.

OFFSETS, ELEVATIONS, AND PERCENT GRADE ARE SHOWN ON THE CROSS SECTIONS.

- ① CONSTRUCTION TOLERANCE OF 0.5%± FOR SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.
- ② THE SIDEWALK RAMP MAXIMUM RUNNING SLOPE SHALL NOT REQUIRE THE RAMP LENGTH TO EXCEED 15 FEET TO AVOID CHASING THE SLOPE INDEFINITELY WHEN CONNECTING TO STEEP GRADES. WHEN APPLYING THE 15 FOOT MAXIMUM LENGTH, THE RUNNING SLOPE OF THE SIDEWALK SHALL BE AS FLAT AS FEASIBLE AND NOT EXCEED THE LONGITUDINAL GRADE OF THE ROADWAY. SLOPE SIDEWALK RAMP TOWARD APRON AS SHOWN WHERE THERE IS NO TERRACE OR WHERE THE TERRACE WIDTH IS LESS THAN 6 FEET WIDE.

- ③ **DRIVEWAY SLOPES: DESIRABLE MAXIMUM**
 10.5% UP AWAY FROM SIDEWALK (SAG)
 8.5% DOWN AWAY FROM SIDEWALK (CREST)
 ABSOLUTE MAXIMUM 15% FOR BOTH CREST AND SAG

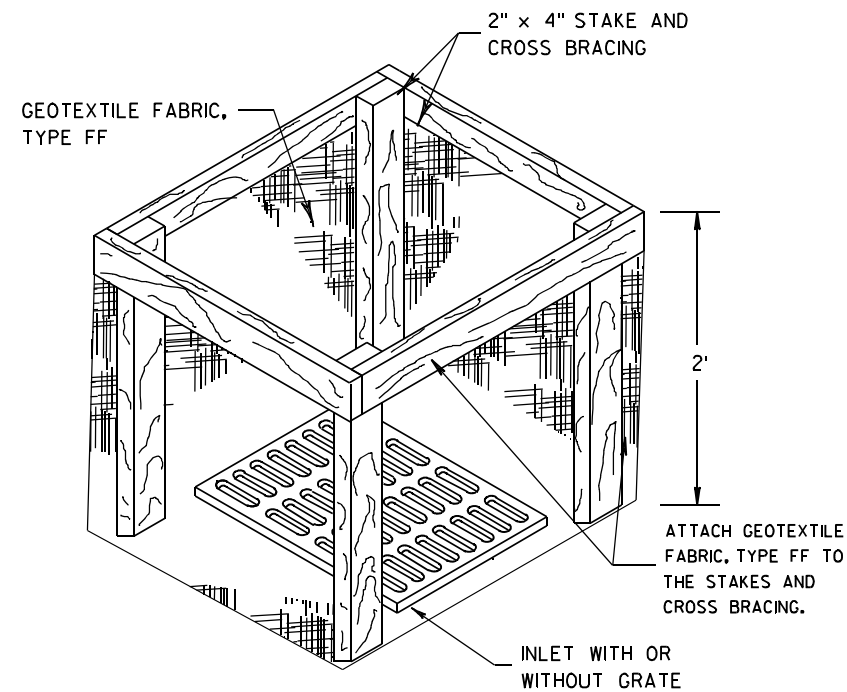
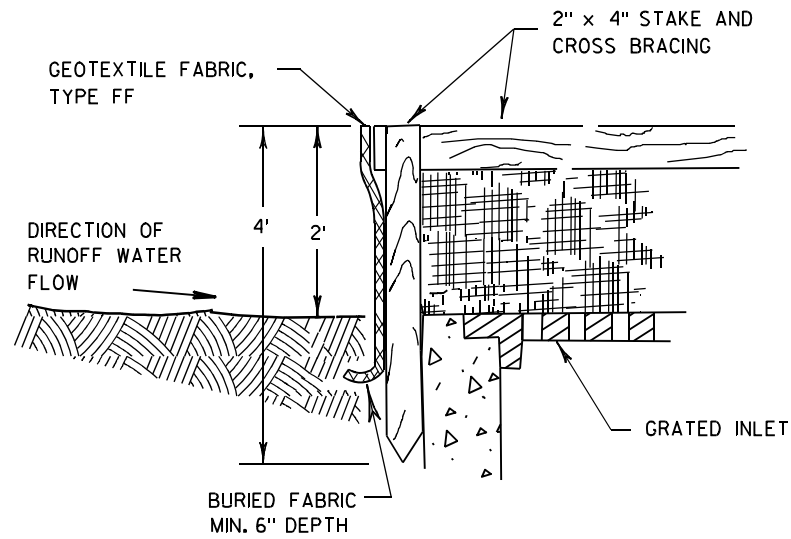
- ④ **DRIVEWAY TYPES**
 • 6-INCH CONCRETE DRIVEWAY PAVEMENT OVER 6-INCH BASE AGGREGATE
 • 2-INCH TO 3-INCH ASPHALTIC SURFACE OVER 6-INCH BASE AGGREGATE
 • 6-INCH BASE AGGREGATE (MAY BE INCREASED FOR CLAY SUBGRADES.)

- ⑤ 1/2" EXPANSION JOINT FILLER

DRIVEWAY AND
SIDWALK RAMPS
TYPES X AND Y

STATE OF WISCONSIN
 DEPARTMENT OF TRANSPORTATION

APPROVED
 February 2022 /S/ Rodney Taylor
 DATE ROADWAY STANDARDS DEVELOPMENT
 ENGINEER



INLET PROTECTION, TYPE A

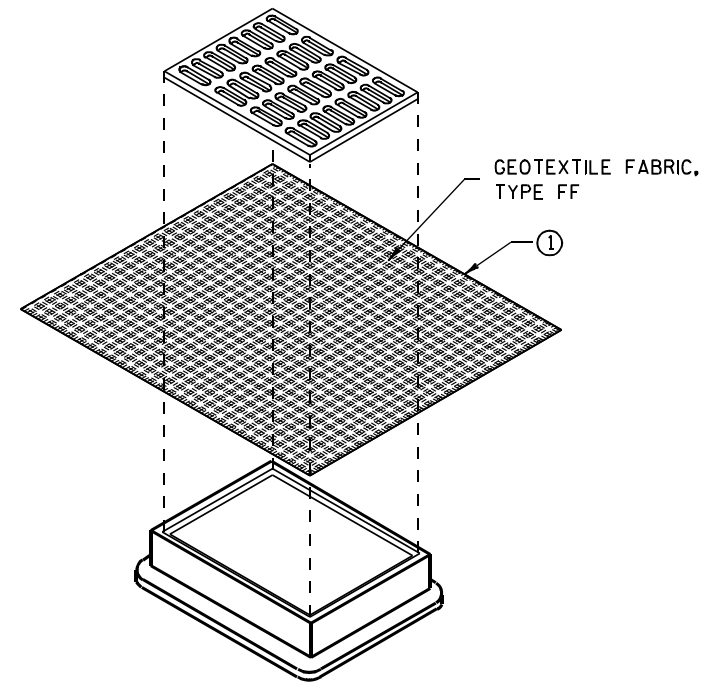
GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

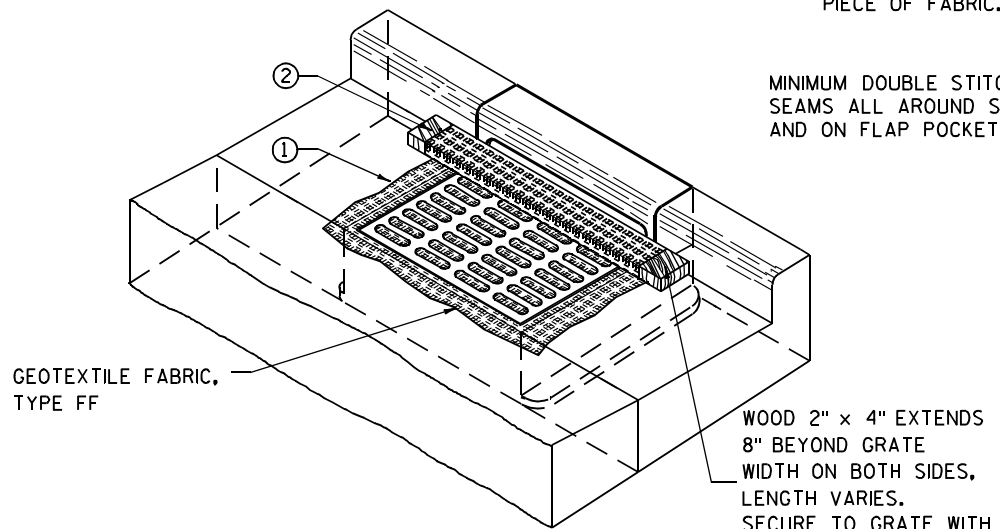
WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- ① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- ② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- ③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



**INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)**

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE C (WITH CURB BOX)

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

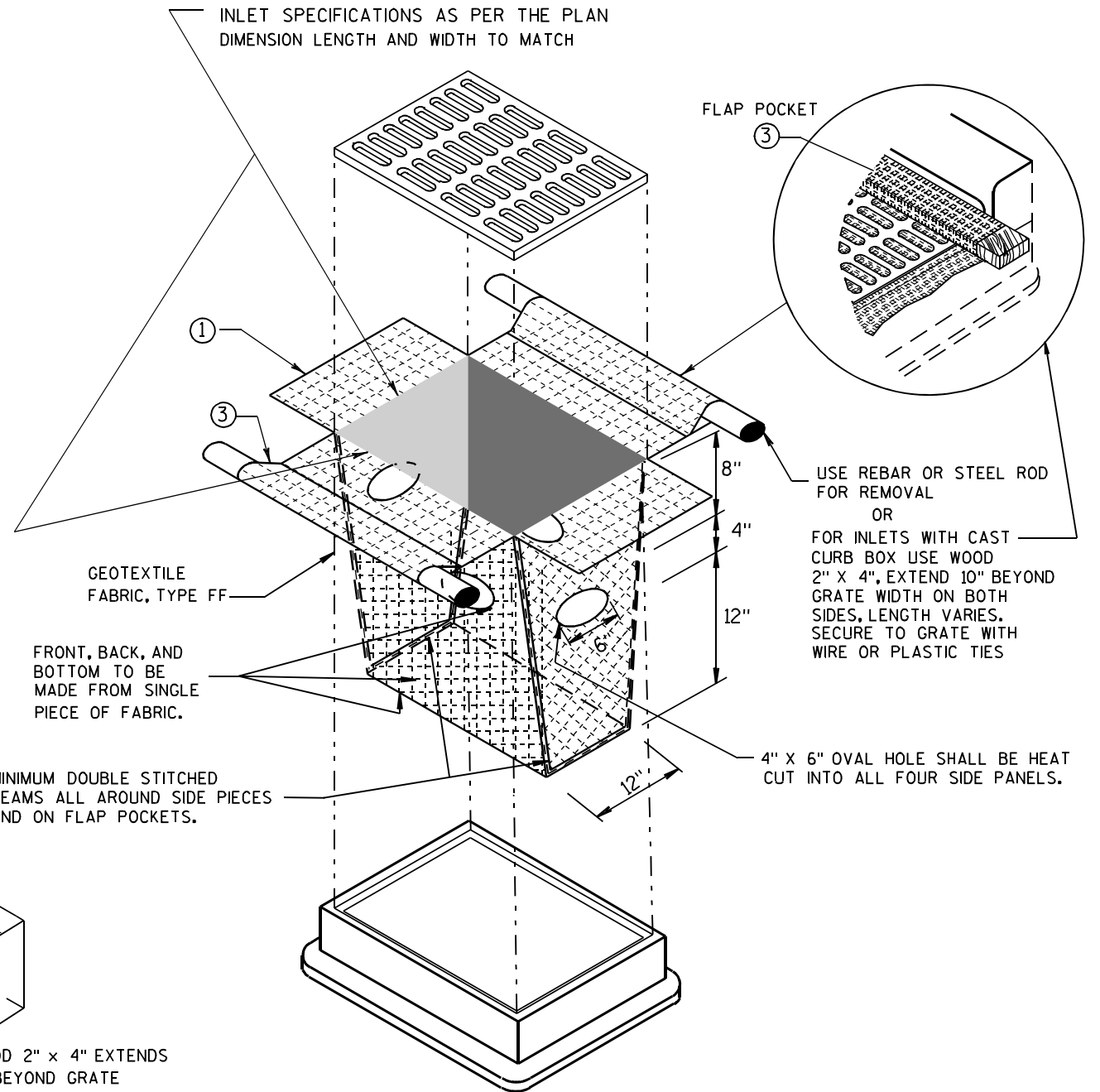
THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

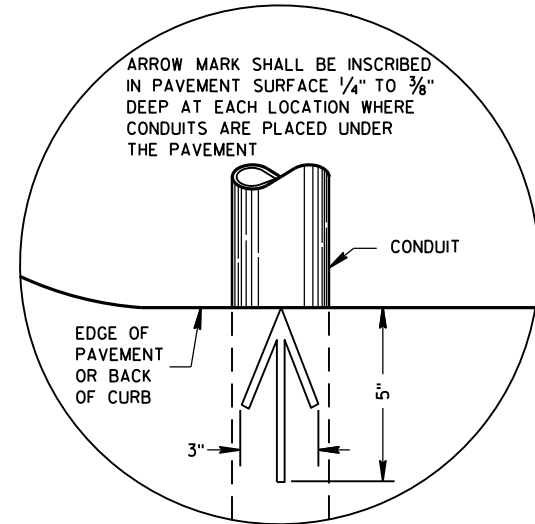
THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.



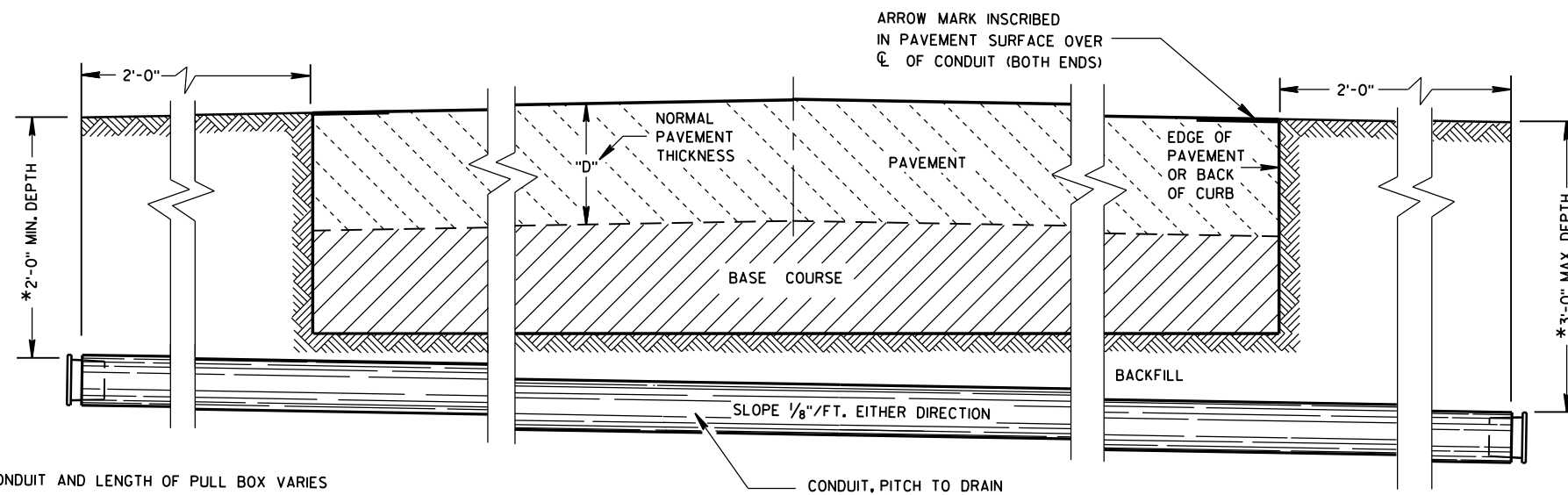
INLET PROTECTION, TYPE D

(CAN BE INSTALLED IN ANY INLET TYPE WITH OR WITHOUT A CURB BOX AS PER NOTE ②)

| | |
|--|--|
| INLET PROTECTION TYPE A, B, C, AND D | |
| STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION | |
| APPROVED 10/16/02 DATE | /s/ Beth Connestra CHIEF ROADWAY DEVELOPMENT ENGINEER |
| FHWA | |



**PLAN VIEW
ARROW MARK**



**SIDE ELEVATION
DETAIL FOR CONDUIT UNDER PAVED HIGHWAYS**

*DEPTH OF CONDUIT AND LENGTH OF PULL BOX VARIES WITH HEIGHT OF CURB USED. ALSO SEE PULL BOX S.D.D. 9B4

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

METALLIC (STANDARD SPECIFICATION 652.2.2) OR NONMETALLIC (STANDARD SPECIFICATION 652.2.3) CONDUIT SHALL BE FURNISHED AND PLACED AS SHOWN.

DEPTH OF CONDUIT INSTALLED BELOW THE TRAVELED WAY SHALL BE 24 INCHES MINIMUM AND 36 INCHES MAXIMUM.

DEPTH OF CONDUIT INSTALLED THAT IS NOT BELOW THE TRAVELED WAY SHALL BE 18 INCHES MINIMUM AND 36 INCHES MAXIMUM.

ANY EXCEPTION TO THE MAXIMUM DEPTH SHALL BE ONLY WITH THE WRITTEN APPROVAL OF THE ENGINEER.

THE TRENCH SHALL NOT BE BACKFILLED PRIOR TO INSPECTION OF THE CONDUIT.

ALL METALLIC CONDUIT RACEWAY ENDS SHALL BE REAMED AND THREADED.

ALL METALLIC CONDUIT IN WHICH WIRE OR CABLE IS TO BE INSTALLED SHALL BE BUSHED WITH APPROVED THREADED BUSHINGS BEFORE INSTALLATION OF THE WIRE OR CABLE.

ALL METALLIC CONDUITS IN WHICH WIRE OR CABLE IS NOT TO BE INSTALLED SHALL BE CAPPED WITH THREADED PROTECTIVE CAPS, AS APPROVED BY THE ENGINEER.

ALL NONMETALLIC CONDUIT SHALL BE CAPPED OR PLUGGED IMMEDIATELY AFTER INSTALLATION AND SHALL REMAIN CAPPED OR PLUGGED UNTIL WIRE/CABLES ARE INSTALLED.

NONMETALLIC CONDUITS IN WHICH WIRE OR CABLE IS NOT BEING INSTALLED SHALL REMAIN CAPPED OR PLUGGED.

BENDING OF PVC ELECTRICAL CONDUIT SHALL BE ACCOMPLISHED BY USING A BLANKET OR EMERSON TYPE TANK DESIGNED FOR THE PURPOSE OF BENDING PVC ELECTRICAL CONDUIT.

ALL CUT ENDS SHALL BE TRIMMED INSIDE AND OUTSIDE TO REMOVE ALL ROUGH EDGES ON NONMETALLIC CONDUIT. (SEE NEC 347.5)

WHEN REQUIRED TO CONNECT NONMETALLIC CONDUIT TO METALLIC CONDUIT, ONLY U.L. LISTED ADAPTER FITTINGS SHALL BE USED.

PRIOR TO CONDUIT ACCEPTANCE, CONDUIT CAPS OR PLUGS SHALL BE REMOVED, AND THE CAPS, PLUGS AND CONDUIT ENDS SHALL BE THOROUGHLY CLEANED AND THEN THE CAPS OR PLUGS REINSTALLED TO ENSURE THAT THE CAPS OR PLUGS CAN BE EASILY REMOVED IN THE FUTURE.

ALL CONDUIT BEING FURNISHED AND INSTALLED SHALL HAVE THE U.L. LABEL FIRMLY ATTACHED.

CONDUIT RUNS SHALL BE THE SAME SIZE OF CONDUIT FROM ONE END TO THE OTHER (FROM PULL BOX TO PULL BOX-OR-JUNCTION BOX TO JUNCTION BOX-OR-BASE TO BASE, ETC.).

TRACER WIRE SHALL BE INSTALLED AS STATED IN THE STANDARD SPECIFICATION, ITEM 652.3.1.1.

ALL CONDUIT RUNS SHALL BE STRAIGHT (WITHOUT BENDS) FROM PULL BOX TO PULL BOX, PULL BOX TO BASE AND BASE TO BASE AS SHOWN ON THE PLANS.

6

6

S.D.D. 9 B 2-10

S.D.D. 9 B 2-10

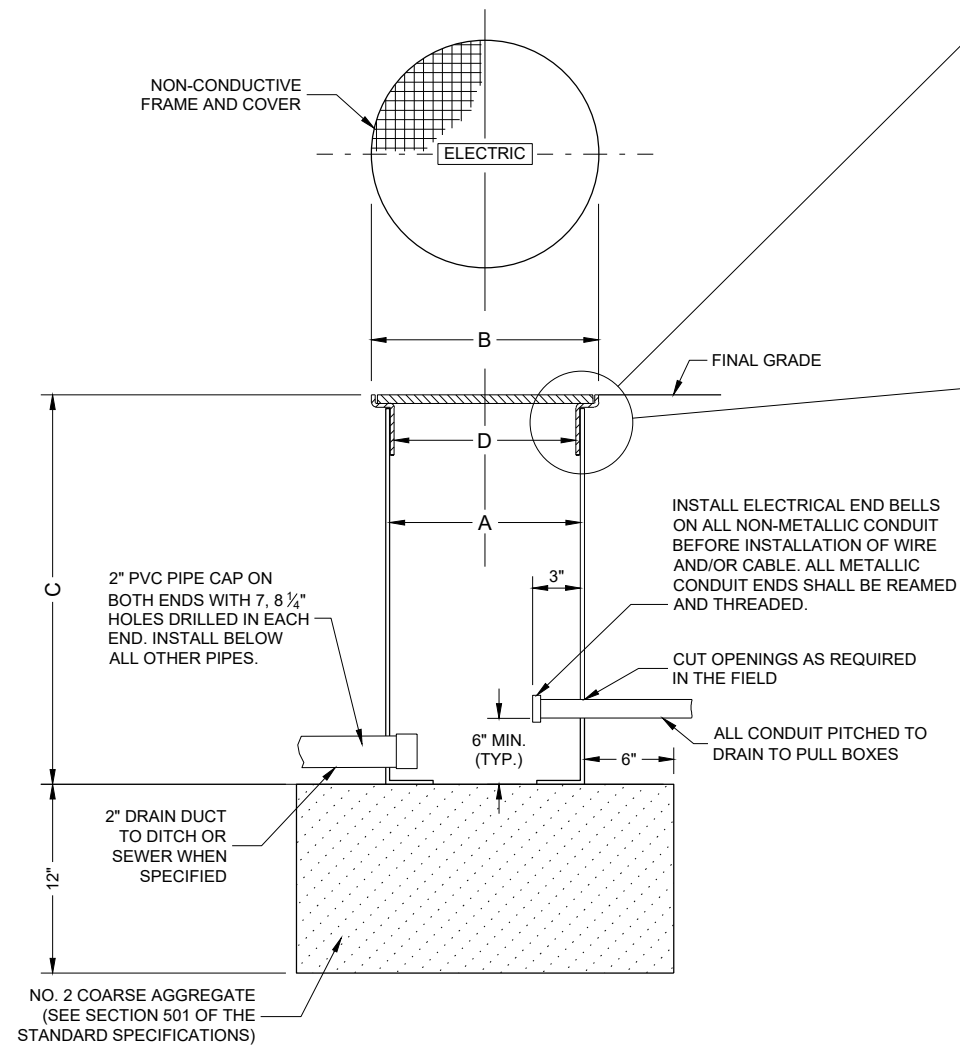
| | |
|--|---|
| CONDUIT | |
| STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION | |
| APPROVED March, 2017 DATE | /S/ Ahmet Demirbilek STATE ELECTRICAL ENGINEER |
| FHWA | |

TABLE OF NOMINAL DIMENSIONS AND WEIGHTS

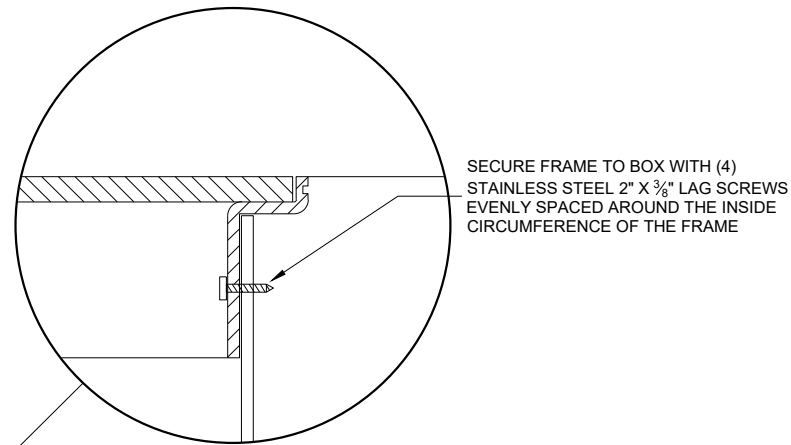
| DIMENSION IN INCHES | | NON- CONDUCTIVE PULL BOX | |
|------------------------------|---|--------------------------|--------|
| BOX DIAMETER ** (INSIDE) | A | 24 | 24 |
| BOX OVERALL OUTSIDE DIAMETER | B | 27 | 27 |
| BOX LENGTH | C | 36 | 42 |
| FRAME OPENING | D | 22 1/2 | 22 1/2 |
| WEIGHT IN POUNDS * | | | |
| COVER | | 50 | 50 |
| BOX ONLY | | 75 | 85 |

* THE ACTUAL WEIGHT OF THE COVER OR BOX ONLY MAY VARY NOT TO EXCEED 100 LBS INDIVIDUALLY.

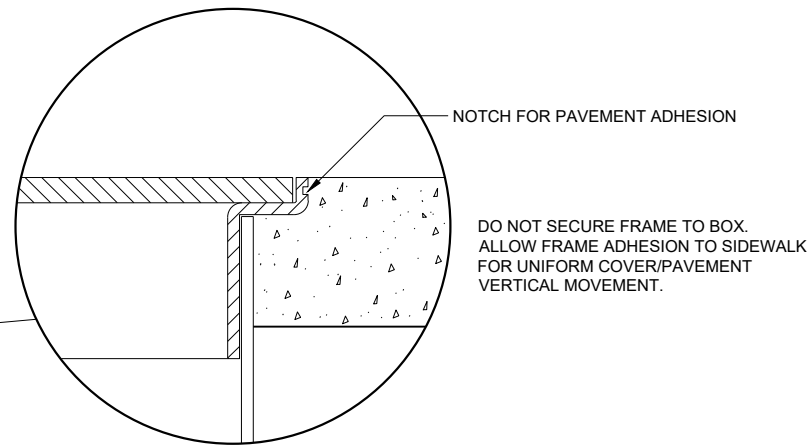
** DIAMETER VARIES FROM TOP TO BOTTOM WITH THE DIAMETER LARGER AT THE BOTTOM TO PREVENT FROST HEAVE.



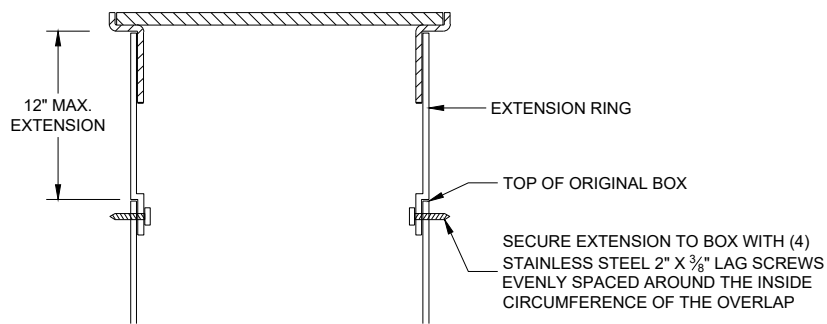
NON-CONDUCTIVE PULL BOX



INSTALLED IN SOD OR CRUSHED AGGREGATE



INSTALLED IN SIDEWALK



BOX EXTENSION

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

ALL BOXES, FRAMES AND COVERS SHALL BE SUITABLE FOR TIER 15 LOADING AS SPECIFIED IN ANSI/SCTE 77.

PROVIDE AN OPENING FOR TOOL ASSISTED COVER REMOVAL NOT LARGE ENOUGH TO PERMIT PASSAGE OF A SPHERE MORE THAN 1/2\"/>

ENSURE COVER SURFACE IS SKID RESISTANT WITH A COEFFICIENT OF FRICTION OF AT LEAST 0.5 AND VERTICAL SURFACE DISCONTINUITIES LESS THAN 1/4\"/>

COVER SHALL BE MAGNETICALLY LOCATABLE.

BOXES AND EXTENSIONS ARE TRIMMABLE FOR CUSTOM LENGTHS. TRIMMED PIECES SHALL MAINTAIN A UNIFORM LENGTH.

ENTRANCE HOLES INTO PULL BOXES SHALL BE CUT WITH A CIRCULAR HOLE SAW OR HYDRAULIC CONDUIT PUNCH. HOLE SIZE SHALL BE THE OUTSIDE DIAMETER OF THE CONDUIT THAT IS TO FIT IN THE OPENING PLUS NO MORE THAN 1/4\"/>

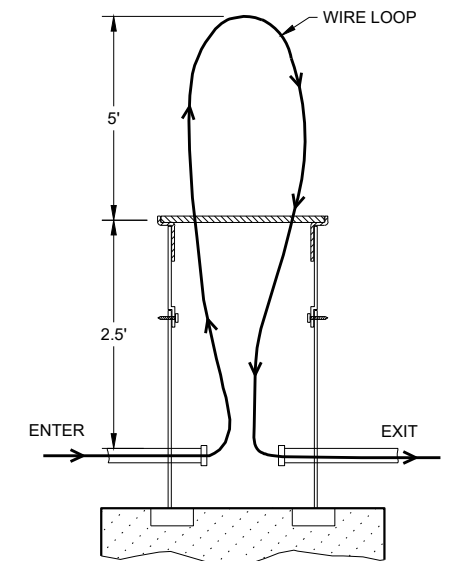
THE CONTRACTOR SHALL NOT INSTALL WIRE IN ANY PULL BOX UNTIL ITS INSTALLATION HAS BEEN INSPECTED AND ACCEPTED BY THE ENGINEER.

ALL METALLIC CONDUIT IN WHICH WIRE AND/OR CABLE IS TO BE INSTALLED, SHALL BE BUSHED BEFORE INSTALLATION OF THE WIRE AND/OR CABLE.

ENTIRE BOX MUST BE CONSTRUCTED OF NON-CONDUCTIVE MATERIALS WITH THE EXCEPTION OF STAINLESS STEEL FASTENERS AND MAGNETIC LOCATABLE DEVICE.

WHEN A PULL BOX IS INSTALLED IN CRUSHED AGGREGATE SHOULDERS, PLACE IT 2-3 INCHES BELOW GRADE AND COVER IT WITH 2-3 INCHES OF CRUSHED AGGREGATE.

LABEL ON COVER SHALL READ "ELECTRIC" FOR SIGNAL AND LIGHTING SYSTEMS, "WISDOT ITS" FOR COMMUNICATIONS AND ITS EQUIPMENT SYSTEMS.



MEASUREMENT DETAIL FOR WIRE/CABLE IN THE PULL BOX

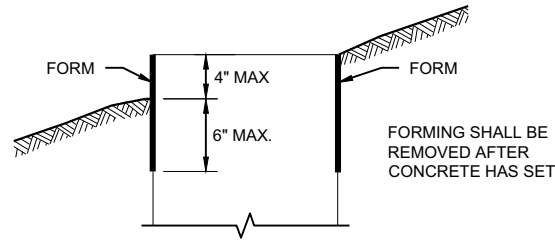
PULL BOXES NON-CONDUCTIVE

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED May 2022 /S/ Ahmet Demirelek DATE STATE ELECTRICAL ENGINEER

FHWA

FORM DEPTH SHALL BE NO MORE THAN 6" BELOW GRADE ON THE LOWER SIDE OF BASE



FORMING DETAIL

| QUANTITY REQUIREMENTS | CONCRETE BASE TYPE | | |
|---------------------------------|--------------------|------|-------|
| | 1 | 2 | 5 & 6 |
| APPROX. CUBIC YARDS OF CONCRETE | 0.40 | 0.57 | 0.40 |
| LBS. OF HOOP BAR STEEL | NONE | 23 | 16 |
| LBS. OF VERTICAL BAR STEEL | NONE | 60 | 18 |

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

BASES SHALL BE EXCAVATED BY USE OF A CIRCULAR AUGER.

TOP SURFACES OF CONCRETE BASES SHALL BE TROWEL FINISHED SMOOTH AND LEVEL.

CONDUIT SIZES AND LOCATIONS SHALL BE SHOWN ON THE PLANS

THE FINAL OR TERMINATING CONCRETE BASE IN A CONDUIT RUN SHALL HAVE A 6" EXIT STUB INSTALLED FOR FUTURE CABLING USE. THE EXIT STUB SHALL BE SIZED AS USED THROUGHOUT THE CONDUIT RUN AS SHOWN AT THE ENTRANCE OF THE BASE.

ENDS OF CONDUIT INSTALLED BELOW GRADE FOR FUTURE USE SHALL BE CAPPED IF METALLIC OR PLUGGED IF NON-METALLIC.

MINIMUM BENDING RADIUS OF CONDUIT IS EQUAL TO 6X THE DIAMETER.

CONDUIT HEIGHT ABOVE CONCRETE BASES SHALL BE 1 INCH. ALL METALLIC CONDUIT ENDS SHALL BE REAMED AND THREADED.

ALL CONDUIT ENDS AT THE TOP OF CONCRETE BASES SHALL BE CAPPED IF METALLIC OR PLUGGED IF NON-METALLIC IMMEDIATELY AFTER PLACEMENT AND BEFORE CONCRETE IS POURED. CONDUITS IN WHICH WIRE OR CABLE IS NOT INSTALLED SHALL REMAIN CAPPED OR PLUGGED.

BELL ENDS SHALL BE INSTALLED ON ALL PVC CONDUIT EXPOSED AT THE TOP OF CONCRETE BASES BEFORE INSTALLATION.

WHEN REQUIRED TO CONNECT NON-METALLIC CONDUIT TO METALLIC CONDUIT, ONLY ADAPTER FITTINGS, U.L. LISTED FOR ELECTRICAL USE, SHALL BE USED.

IF A BASE REQUIRES A DEEP FORM BECAUSE OF LOOSE DIRT OR FILL, THE FORM SHALL BE REMOVED BEFORE BACKFILLING AROUND THE BASE. BACKFILL SHALL BE TAMPED TIGHT AGAINST THE BARE CONCRETE BASE IN LAYERS OF 1 FOOT OR LESS.

A NO. 4 AWG STRANDED COPPER EQUIPMENT GROUNDING CONDUCTOR SHALL BE EXOTHERMICALLY WELDED TO THE EQUIPMENT GROUNDING ELECTRODE (GROUND ROD) FOR TYPE 2, TYPE 5 AND TYPE 6 BASES.

THE EQUIPMENT GROUNDING CONDUCTOR SHALL BE FURNISHED AND INSTALLED TO ENTER ALL BASE TYPES THROUGH A 1 INCH CONDUIT INSTALLED FOR GROUNDING PURPOSES, LEAVING A 4 FOOT COIL OF WIRE ABOVE THE CONCRETE BASE. THE EQUIPMENT GROUNDING CONDUCTOR SHALL BE NEATLY COILED AND THE COILS TIED TOGETHER.

ANCHOR RODS SHALL BE THREADED 12" IN LENGTH ON EACH END OF THE ROD. ANCHOR RODS SHALL BE MANUFACTURED IN ACCORDANCE WITH SECTION 654.2.1 OF THE STANDARD SPECIFICATIONS.

WASHERS AND LOCK WASHERS ARE REQUIRED ON ALL ANCHOR RODS.

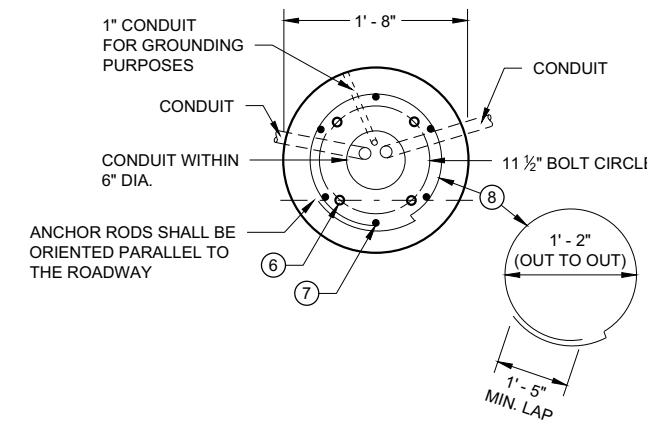
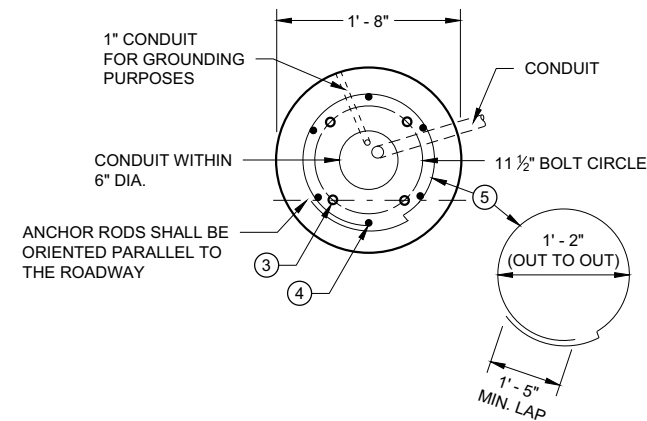
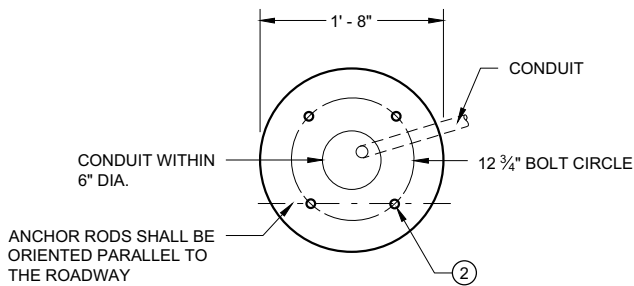
WHEN ANCHOR RODS USING THE ALTERNATE "L" BEND ARE FURNISHED, THE 4 INCH "L" BEND SHALL BE IN ADDITION TO THE SPECIFIED ANCHOR ROD BAR LENGTH. THE "L" BEND SHALL NOT BE THREADED.

ANCHOR RODS SHALL BE INSTALLED WITH MISALIGNMENTS OF LESS THAN 1:40 FROM VERTICAL.

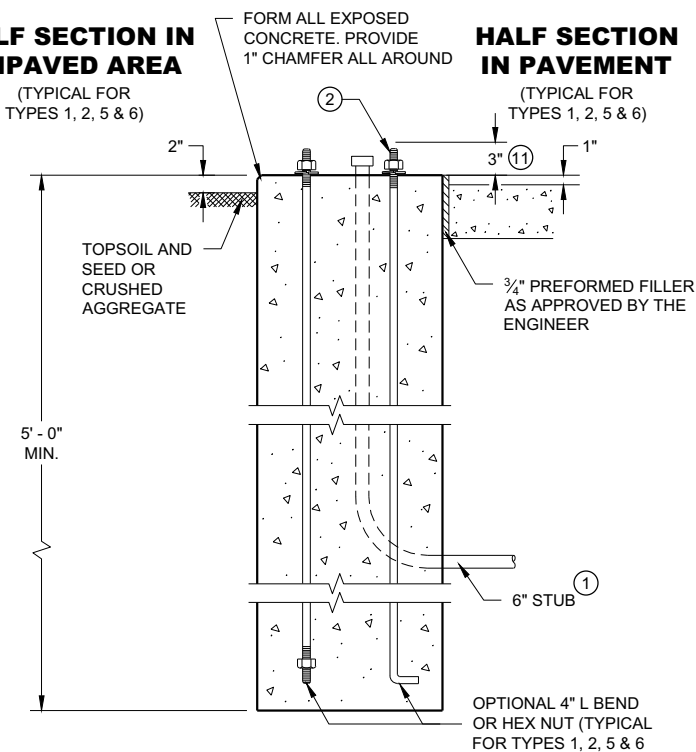
WELDING OF THE ANCHOR RODS TO THE CAGE IS UNACCEPTABLE. TIE WIRES SHALL BE USED.

BAR STEEL REINFORCEMENT SHALL BE COATED WITH POWDERED EPOXY RESIN IN ACCORDANCE WITH SECTION 505 OF THE STANDARD SPECIFICATIONS (LATEST EDITION).

- ① THE MINIMUM DEPTH OF CONDUIT EXITING THE CONCRETE BASE AND INSTALLED BELOW THE TRAVELED WAY SHALL BE 24 INCHES. THE MINIMUM DEPTH OF CONDUIT EXITING THE CONCRETE BASE THAT IS NOT INSTALLED BELOW THE TRAVELED WAY SHALL BE 18 INCHES. THE MAXIMUM DEPTH OF ALL CONDUIT SHALL BE 36 INCHES EXCEPT WITH WRITTEN APPROVAL OF THE ENGINEER.
- ② (4) 1" DIA. X 3' - 6" ANCHOR RODS.
- ③ (4) 1" DIA. X 5' - 0" ANCHOR RODS.
- ④ (6) NO. 6 X 6' - 8" BAR STEEL REINFORCEMENT.
- ⑤ (7) NO. 4 X 5' - 1" BAR STEEL REINFORCEMENT @ 1' - 0" C - C.
- ⑥ (4) 1" DIA. X 3' - 6" ANCHOR RODS.
- ⑦ (6) NO. 4 X 4' - 8" BAR STEEL REINFORCEMENT.
- ⑧ (5) NO. 4 X 5' - 1" BAR STEEL REINFORCEMENT @ 1' - 0" C - C.
- ⑨ EXOTHERMIC CONNECTION TO EQUIPMENT GROUNDING CONDUCTOR
- ⑩ 5/8" DIA. X 8' - 0" COPPERCLAD EQUIPMENT GROUNDING ELECTRODE REQUIRED
- ⑪ ANY ANCHOR ROD PROJECTION SHORTER THAN 2 3/4" OR LONGER THAN 3 1/4" SHALL REQUIRE THE BASE TO BE REMOVED AND REPLACED AT THE CONTRACTORS EXPENSE.
- ⑫ FOR NON - BREAKAWAY INSTALLATIONS, 4 1/2" ± ANCHOR ROD PROJECTION WITH THE USE OF LEVELING NUTS. RODENT SCREEN REQUIRED.

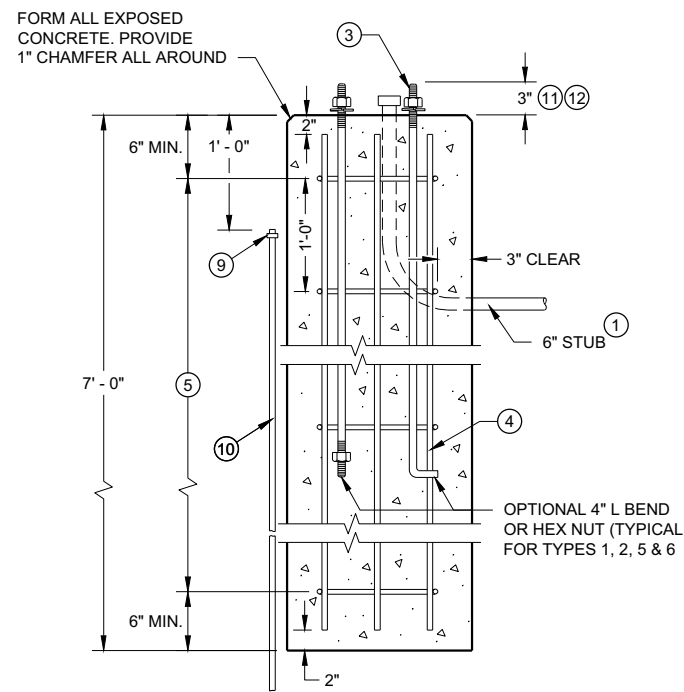


HALF SECTION IN UNPAVED AREA

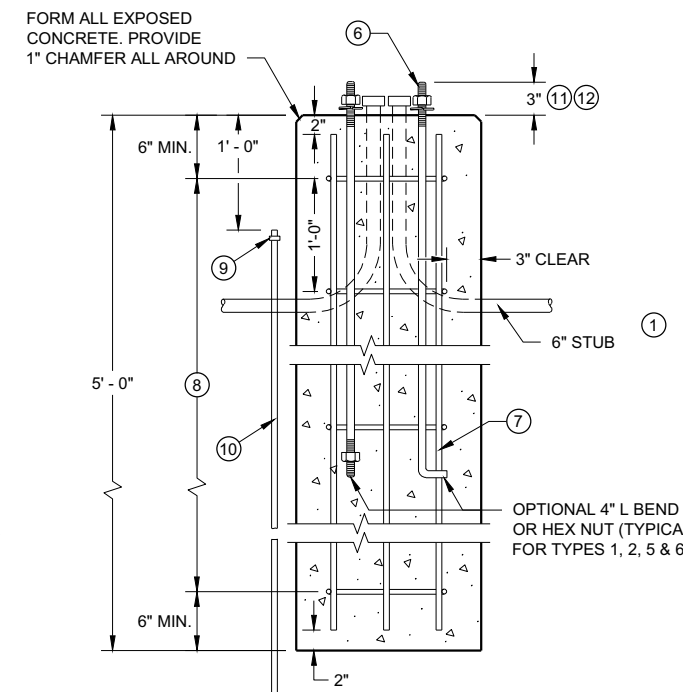


TYPE 1

HALF SECTION IN PAVEMENT



TYPE 2



TYPE 5 & 6

CONCRETE BASES

**CONCRETE BASES
TYPES 1, 2, 5, & 6**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2019 /S/ Ahmet Demirelek
DATE STATE ELECTRICAL ENGINEER

FHWA

6

6

SDD 09C02 - 09

SDD 09C02 - 09

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

FOUR (4) BOLTS SHALL BE FURNISHED WITH EACH TRANSFORMER BASE. BOLTS SHALL BE 1" DIAMETER, 4" IN LENGTH, WITH WASHERS, LOCK WASHERS AND NUTS. BOLTS, NUTS AND WASHERS SHALL BE MANUFACTURED IN ACCORDANCE WITH SECTION 531.2.2 OF THE STANDARD SPECIFICATIONS.

LEVELING SHIMS, IF NEEDED, SHALL BE DESIGNED FOR THE PURPOSE AND USED UNDER CAST BASES WHEN PLUMBING POLES OR STANDARDS DURING INSTALLATION. THE USE OF WASHERS IN LIEU OF PROPER LEVELING SHIMS IS NOT ACCEPTABLE.

SHIM LENGTH SHALL BE LONG ENOUGH TO COMPLETELY COVER THE AREA UNDER THE LENGTH AND WIDTH OF THE BASE MOUNTING FLANGE.

DOUBLE NUTTING IS NOT ACCEPTABLE FOR LEVELING OR MOUNTING PURPOSES.

A NEMA APPROVED, U.L. LISTED, COPPER WITH BRASS OR STAINLESS STEEL SET SCREW, DIRECT BURY RATED, MECHANICAL CONNECTOR (LUG), SIZED TO ACCEPT AWG. #10 TO #4 COPPER STRANDED WIRE SHALL BE FURNISHED AND INSTALLED IN THE PEDESTAL AND TRANSFORMER BASES.

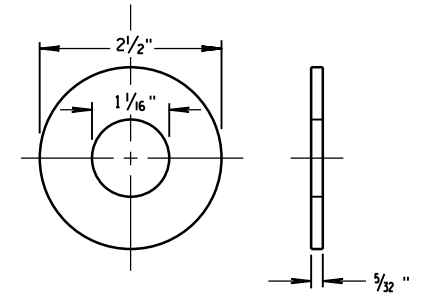
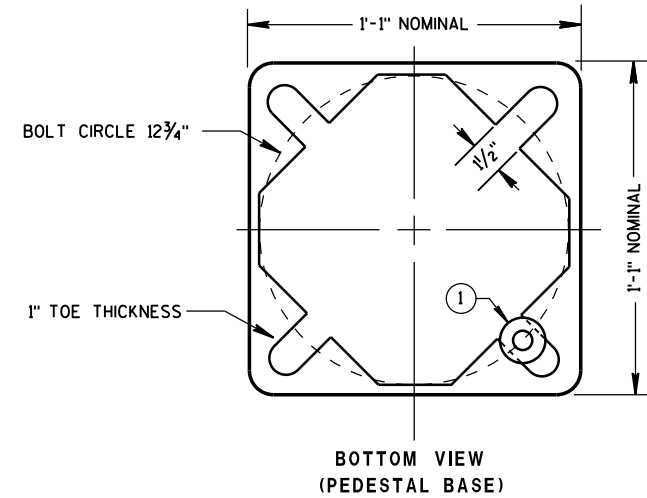
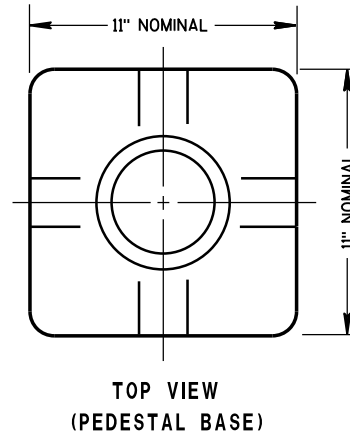
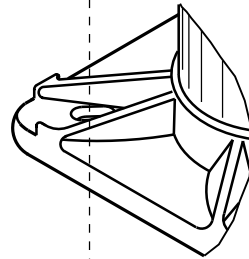
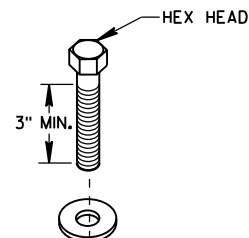
THE MECHANICAL CONNECTOR SHALL BE INSTALLED USING A 1/4" - 20 (TPI) STAINLESS STEEL HEX HEAD BOLT OF SUFFICIENT LENGTH TO FIRMLY ATTACH THE LUG TO THE BASE.

SHOULD THE MANNER OF ATTACHMENT OF THE LUG REQUIRE WASHERS, HEX NUTS, LOCK WASHER - THEY SHALL BE STAINLESS STEEL AS IS THE BOLT. THE MANNER OF ATTACHMENT SHALL NOT BLOCK ACCESSIBILITY TO WIRE PLACEMENT IN THE CONNECTOR.

PEDESTAL BASE COLLAR THREADING SHALL BE TAPERED AND IN ACCORDANCE WITH NATIONAL PIPE THREADING DIMENSIONS.

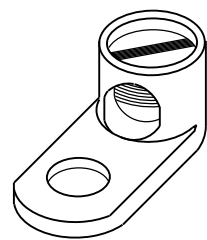
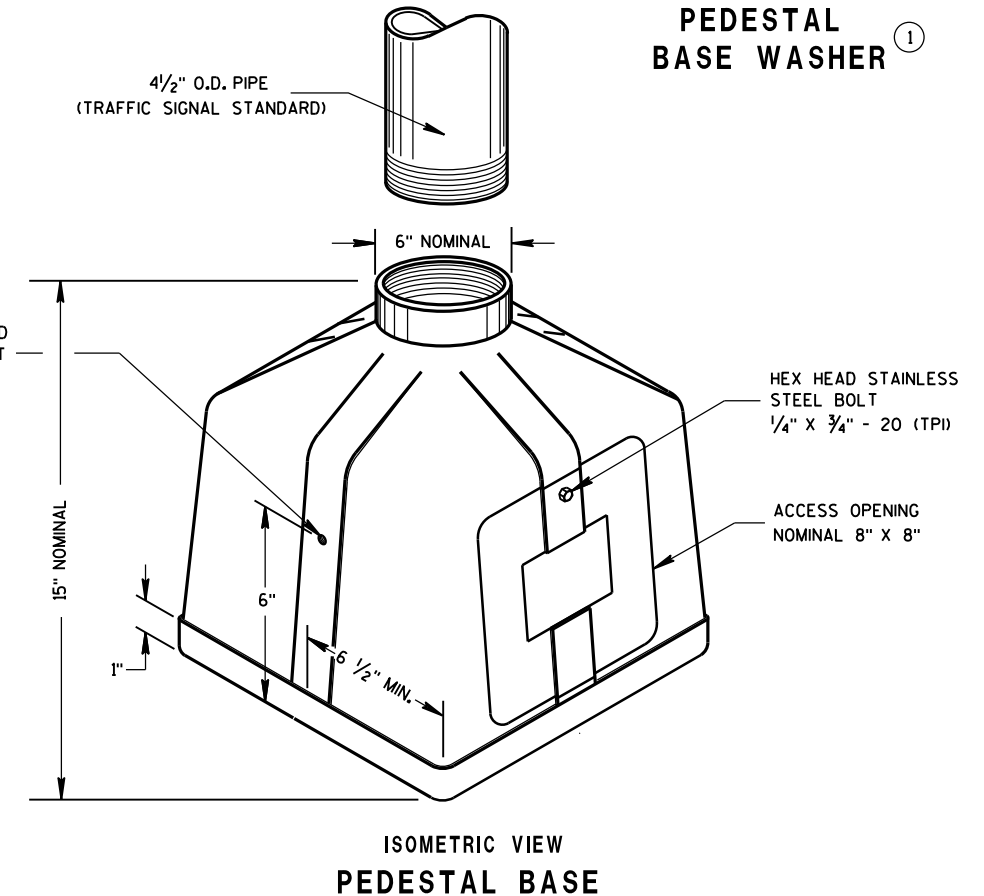
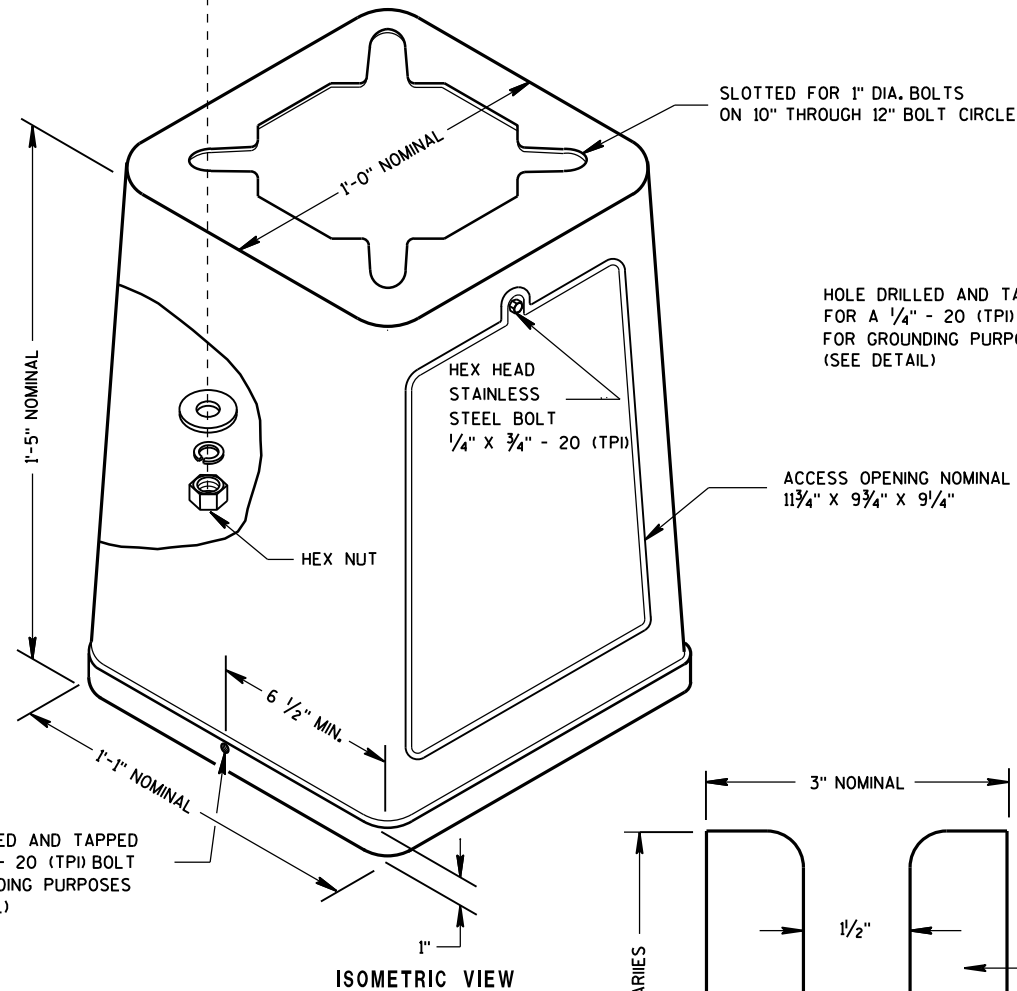
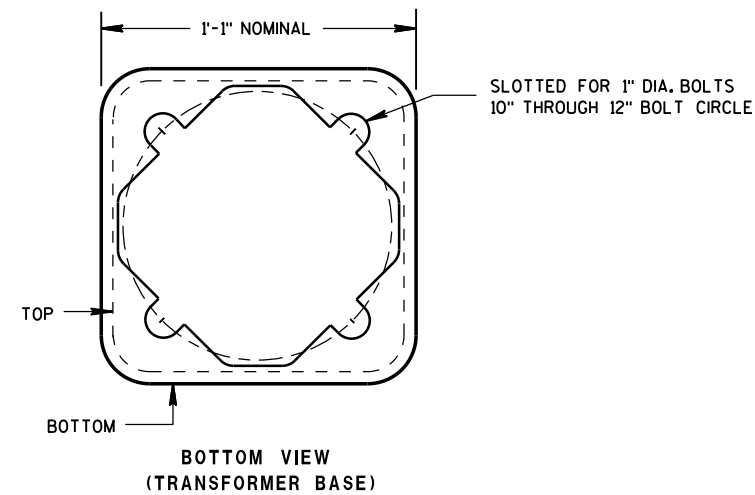
BASE COLLAR THREADING SHALL EXTEND INTO THE BASE COLLAR WITH SUFFICIENT DEPTH TO ACCEPT THE INSTALLATION OF TRAFFIC SIGNAL STANDARDS TO A DEPTH OF 1/2", THEN TIGHTENING TO A POINT OF BEING IMMOVABLE.

THE ACCESS DOOR SHALL BE OF THE SAME MATERIAL AS THE BASE.



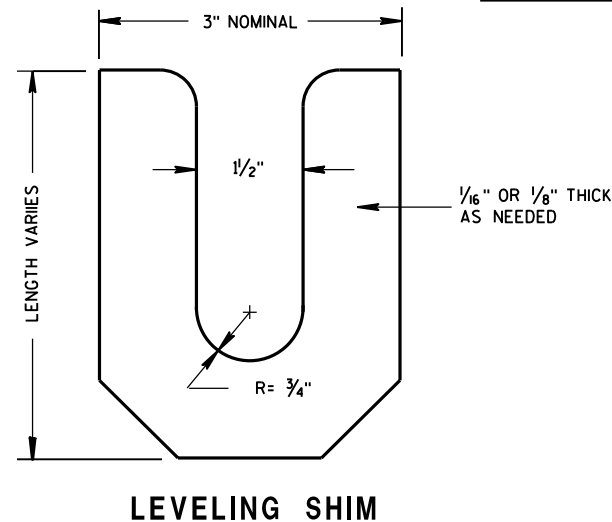
ZINC COATED STEEL WASHER TO BE PROVIDED BY THE CONTRACTOR

PEDESTAL BASE WASHER ①



TYPICAL MECHANICAL CONNECTOR LUG
TO BE FURNISHED WITH EACH BASE

TRANSFORMER BASE
INTENDED FOR USE WITH TYPE 2, 3, 4, 5 & 6 POLES



LEVELING SHIM

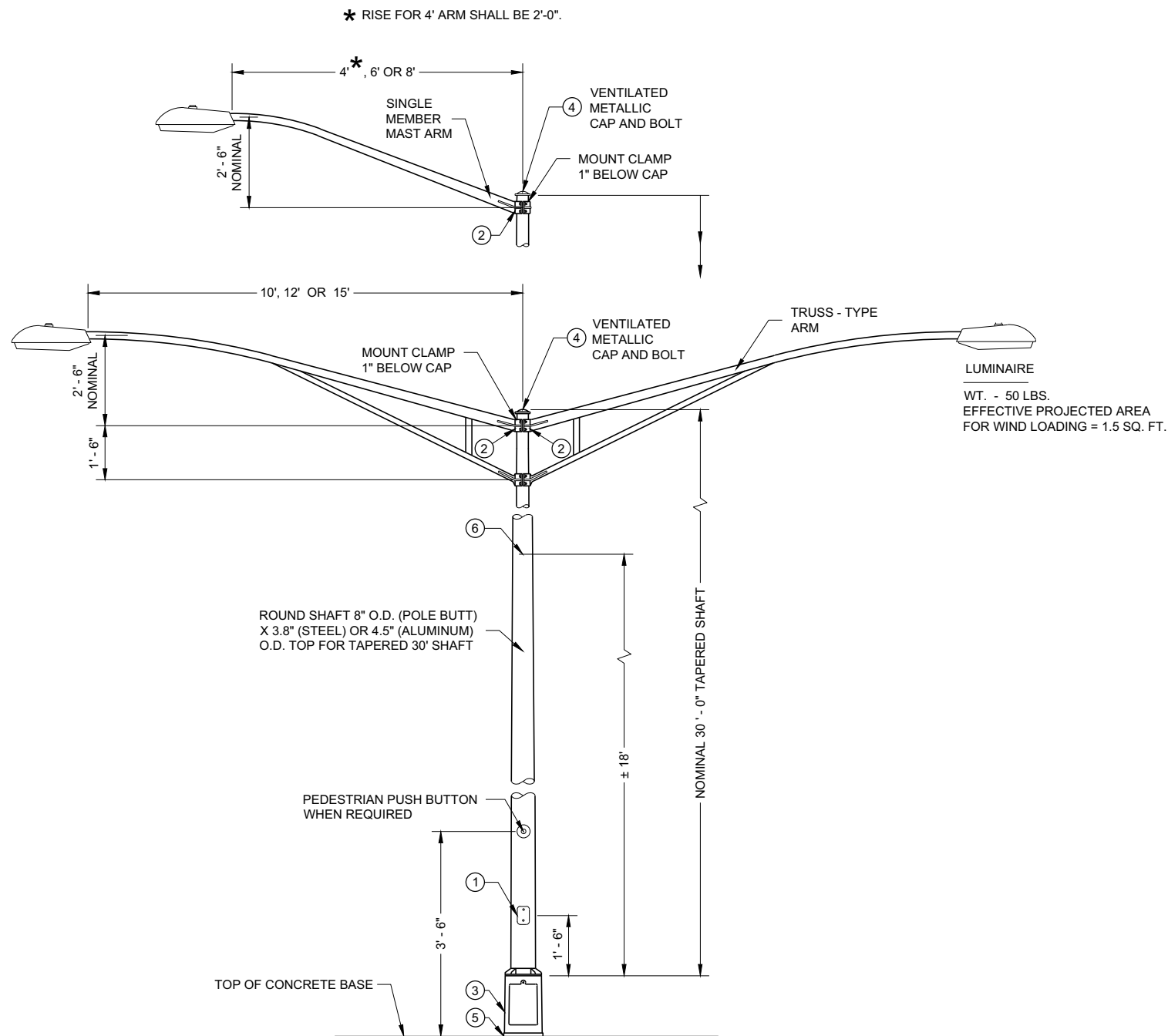
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S.D.D. 9 C 3-4

S.D.D. 9 C 3-4

| | |
|--|---|
| TRANSFORMER/PEDESTAL BASES | |
| STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION | |
| APPROVED Sept. 2014 DATE | /S/ Ahmet Demirbilek STATE ELECTRICAL ENGINEER |
| FHWA | |



**TYPE 5 POLE MOUNTING CONFIGURATION
(MAXIMUM LOAD)
LIGHTING ONLY**

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

SECTION 657, POLES, OF THE STANDARD SPECIFICATIONS SHALL APPLY TO THIS DRAWING.

ALL TYPE 5 POLE MOUNTINGS SHALL BE DESIGNED TO INCLUDE TWIN 15' ARMS WITH LUMINAIRES.

POLES SHALL BE GALVANIZED STEEL OR ALUMINUM, AS CALLED FOR IN THE CONTRACT.

TYPE 5 ALUMINUM POLES SHALL BE CONSTRUCTED OF 6063 - T6 ALUMINUM ALLOY. SLEEVING INSIDE THE POLE IS NOT ACCEPTABLE.

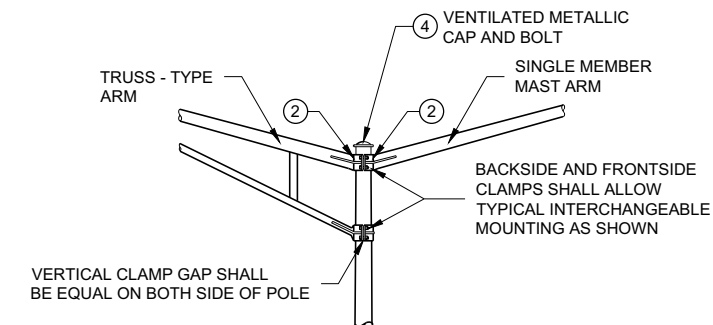
TYPE 5 ALUMINUM POLES SHALL HAVE A MINIMUM WALL THICKNESS OF 0.1888".

TYPE 5 STEEL POLES SHALL HAVE A MINIMUM WALL THICKNESS OF U.S. STANDARD 11 GAGE (0.1196").

THE SLIPFITTER END OF THE LUMINAIRE MAST ARM SHALL BE A NOMINAL 2 3/8 INCHES IN OUTSIDE DIAMETER. THE STRAIGHT PORTION OF THE SLIPFITTER END OF THE LUMINAIRE MAST ARM SHALL BE A NOMINAL 12 INCHES IN LENGTH.

WHEN TRANSFORMER BASES ARE USED, WIRE CONNECTIONS SHALL BE MADE IN THE TRANSFORMER BASE.

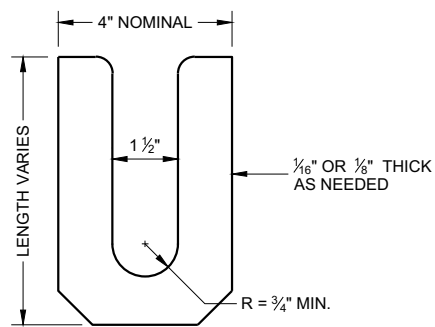
- ① 4" X 6" REINFORCED HANDHOLE AND COVER ASSEMBLY WITH TWO (2) 1/4" X 3/4" - 20 TPI, STAINLESS STEEL, HEX HEAD BOLTS.
- ② GROMMETS. 1" CHASE NIPPLES OR 1" CLOSE CONDUIT NIPPLES WITH BUSHINGS SHALL BE PROVIDED FOR 1 3/8" HOLE IN POLE SHAFT FOR WIRING.
- ③ CAST ALUMINUM TRANSFORMER BASE, WHEN REQUIRED.
- ④ FURNISH AND INSTALL VENTILATED, CAST METALLIC (ALUMINUM ALLOY) CAPS. FASTEN CAPS WITH ONE (1) 1/4" X 3/4" - 20 TPI STAINLESS STEEL, HEX HEAD BOLT.
- ⑤ SHIMMING, IF NEEDED, SHALL BE LOCATED BETWEEN THE CONCRETE FOUNDATION AND POLE.
- ⑥ INTERNAL DUMBBELL - TYPE VIBRATION DAMPER.



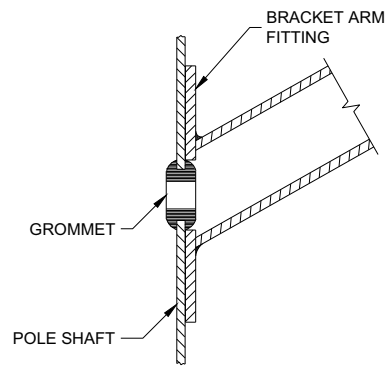
INTERCHANGEABLE MOUNTING DETAIL

**POLE MOUNTINGS FOR
LIGHTING UNITS, TYPE 5
(30 FEET)**

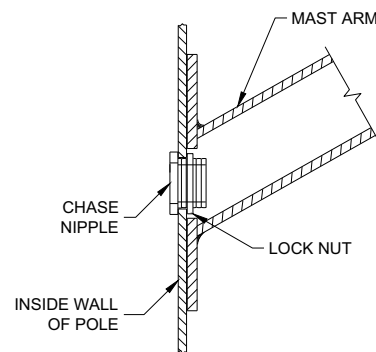
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



LEVELING SHIM
SHALL BE ALUMINUM



TYPICAL APPLICATION OF GROMMET IN POLE SHAFT



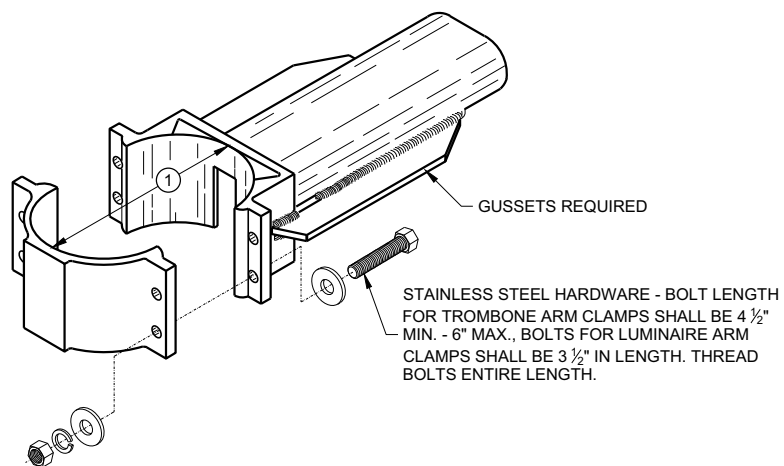
TYPICAL APPLICATION OF CHASE NIPPLE IN POLE SHAFT

GENERAL NOTES

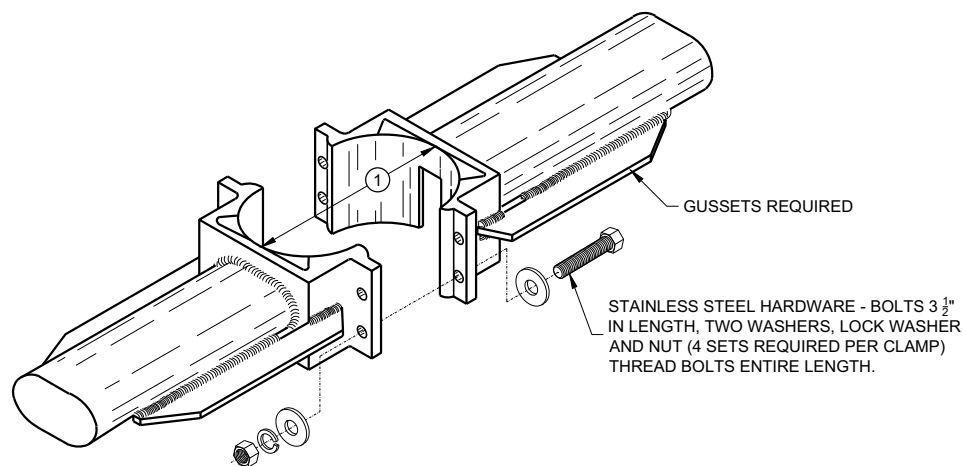
CLAMP BOLT-NUT TIGHTENING TORQUE SHALL BE INDICATED BY INDENT STAMPING (1/2 INCH NUMERALS AND LETTERS) OR WEATHERPROOF PRINTING ON THE INSIDE OF THE CLAMP THAT IS WELDED TO THE ARM MEMBER.

- ① 4.5" I.D. FOR LUMINAIRE MAST ARM CLAMP. 6.625" I.D. FOR TROMBONE MAST ARM CLAMP.
- ② INDIVIDUAL BASE PLATE ANCHOR ROD COVERS. (4 REQUIRED)
- ③ BASE PLATE SLOTTED TO ACCEPT 11" THROUGH 12" BOLT CIRCLE USING 1" DIAMETER ANCHOR RODS.
- ④ LEVELING SHIMS, DESIGNED FOR THE PURPOSE, SHALL BE USED WHEN PLUMBING POLES. THE USE OF WASHERS IN LIEU OF PROPER LEVELING SHIMS IS NOT ACCEPTABLE. LEVELING SHIMS SHALL BE USED ONLY BETWEEN THE TOP OF THE CONCRETE BASE AND A METALLIC BASE PLATE.

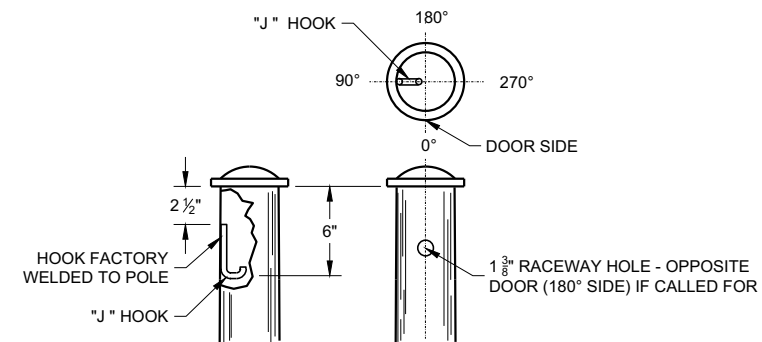
SHIMS SHALL BE LONG ENOUGH AND WIDE ENOUGH TO COMPLETELY COVER THE AREA UNDER THE LENGTH AND WIDTH OF THE BASE MOUNTING FLANGE.



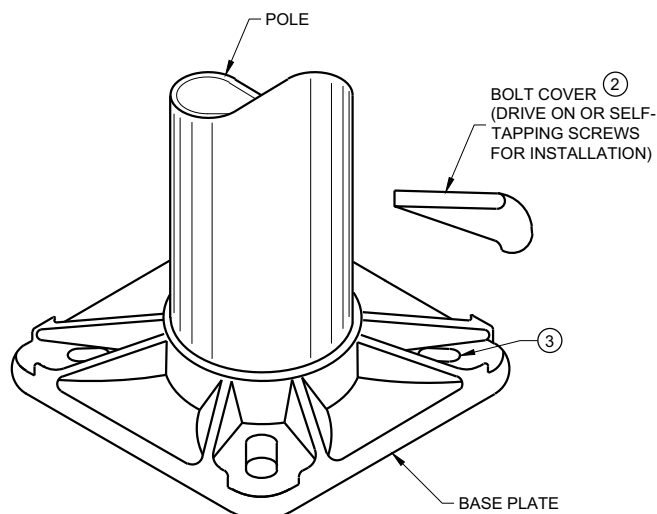
TYPICAL TROMBONE MAST ARM AND SINGLE LUMINAIRE MAST ARM MOUNTING CLAMP



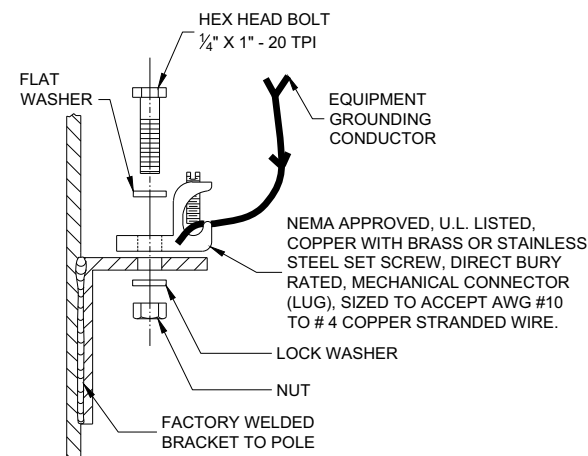
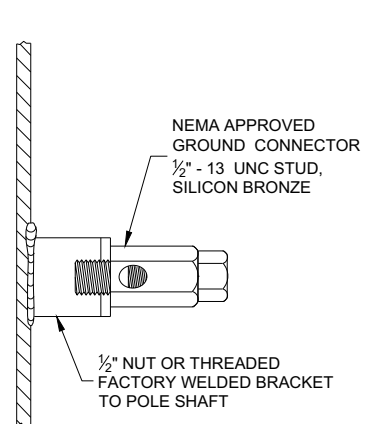
TYPICAL LUMINAIRE MAST ARM (DOUBLE) MOUNTING BRACKETS



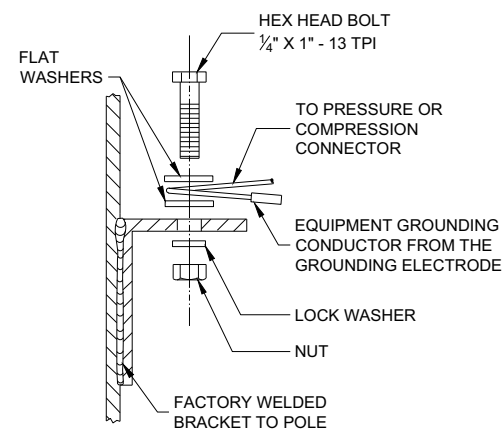
TYPICAL "J" HOOK LOCATION



BASE PLATE



TYPICAL GROUNDING CONNECTIONS
NUT, BOLT AND WASHERS SHALL BE STAINLESS STEEL



HARDWARE DETAILS FOR POLE MOUNTING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2018 /S/ Ahmet Demirelek
DATE STATE ELECTRICAL ENGINEER

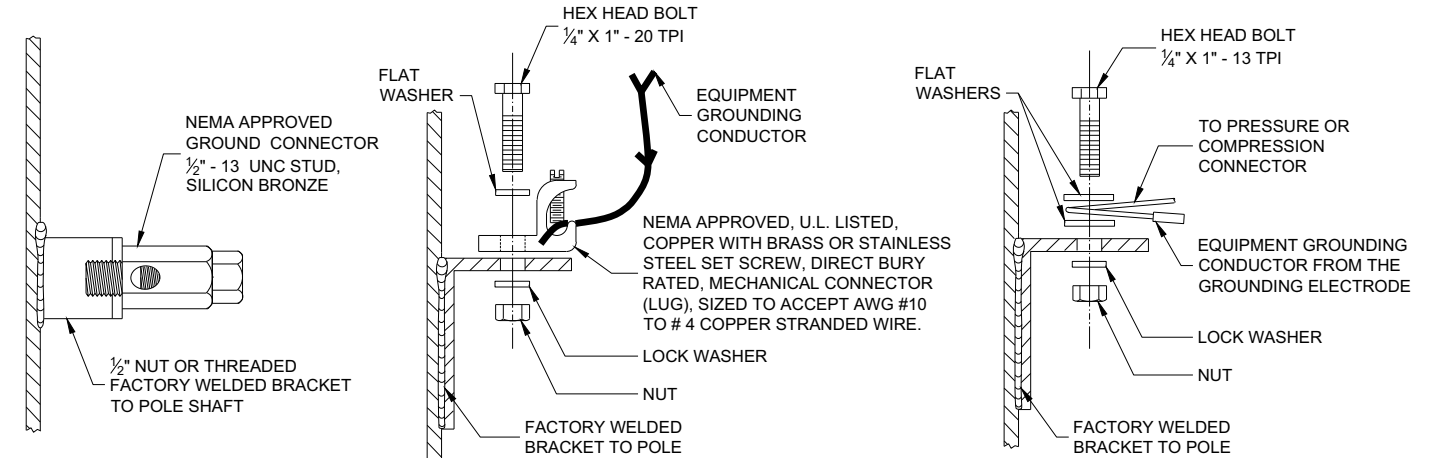
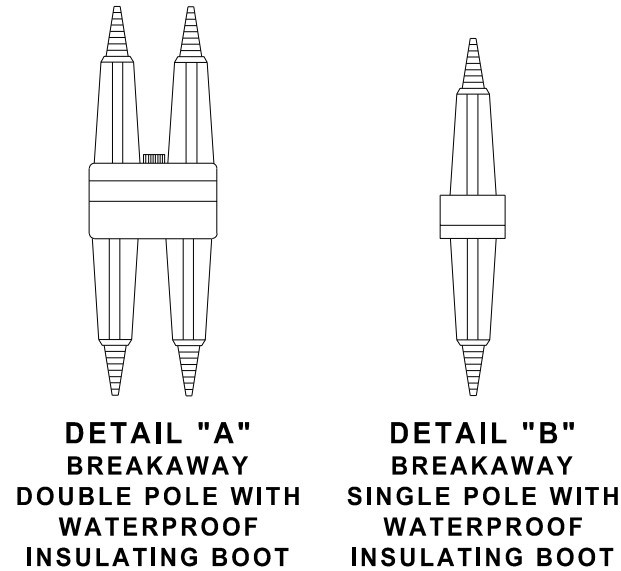
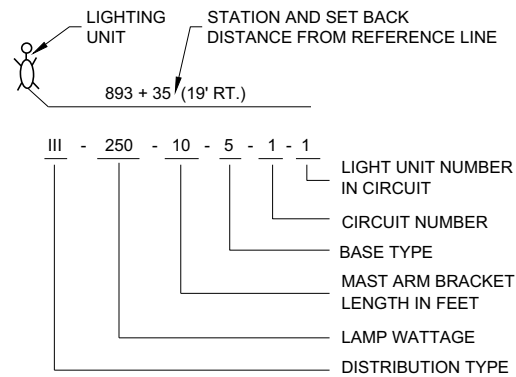
FHWA

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

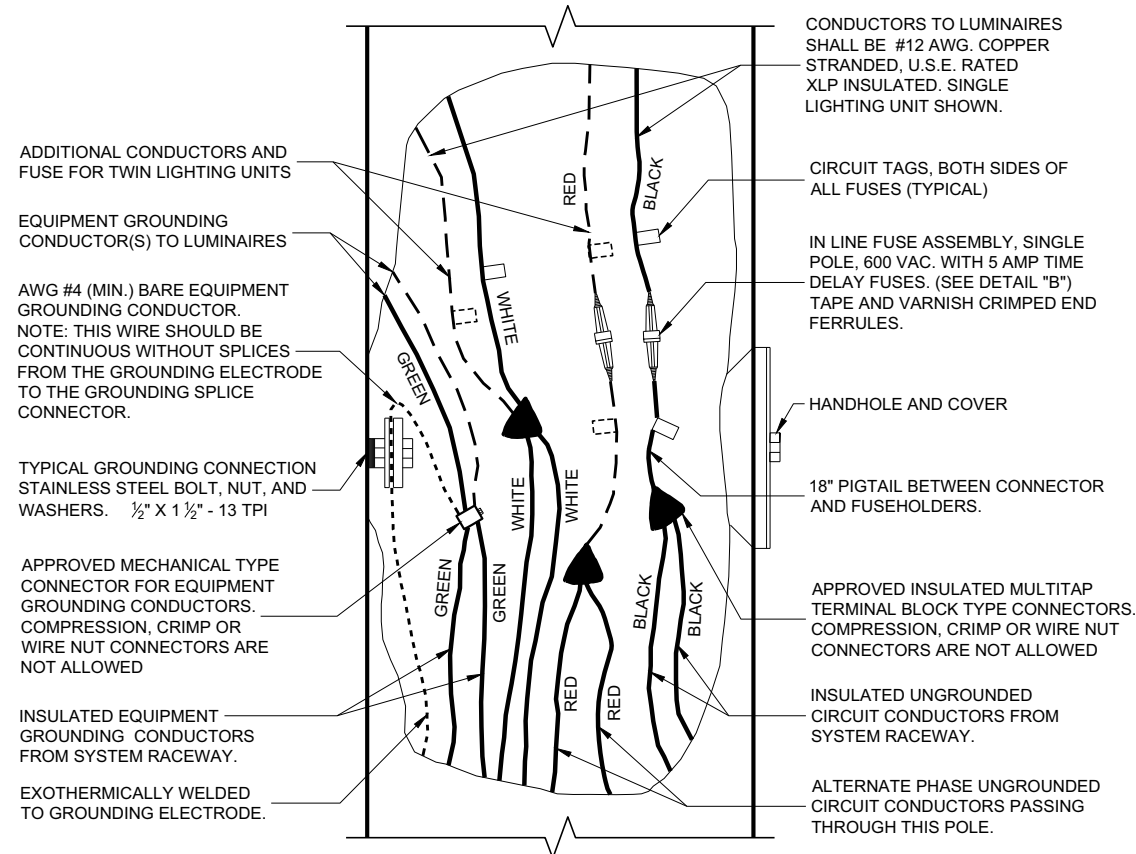
THE EQUIPMENT GROUND CONNECTOR SHALL BE TAPED WITH 3 WRAPS (MINIMUM) OF APPROVED RUBBER TAPE AND 3 WRAPS (MINIMUM) OF APPROVED VINYL TAPE TO COVER SHARP WIRE ENDS AFTER THE CONNECTION IS COMPLETED.

WHEN TRANSFORMER BASES ARE USED, ALL WIRING CONNECTIONS SHALL OCCUR WITHIN THE TRANSFORMER BASES.

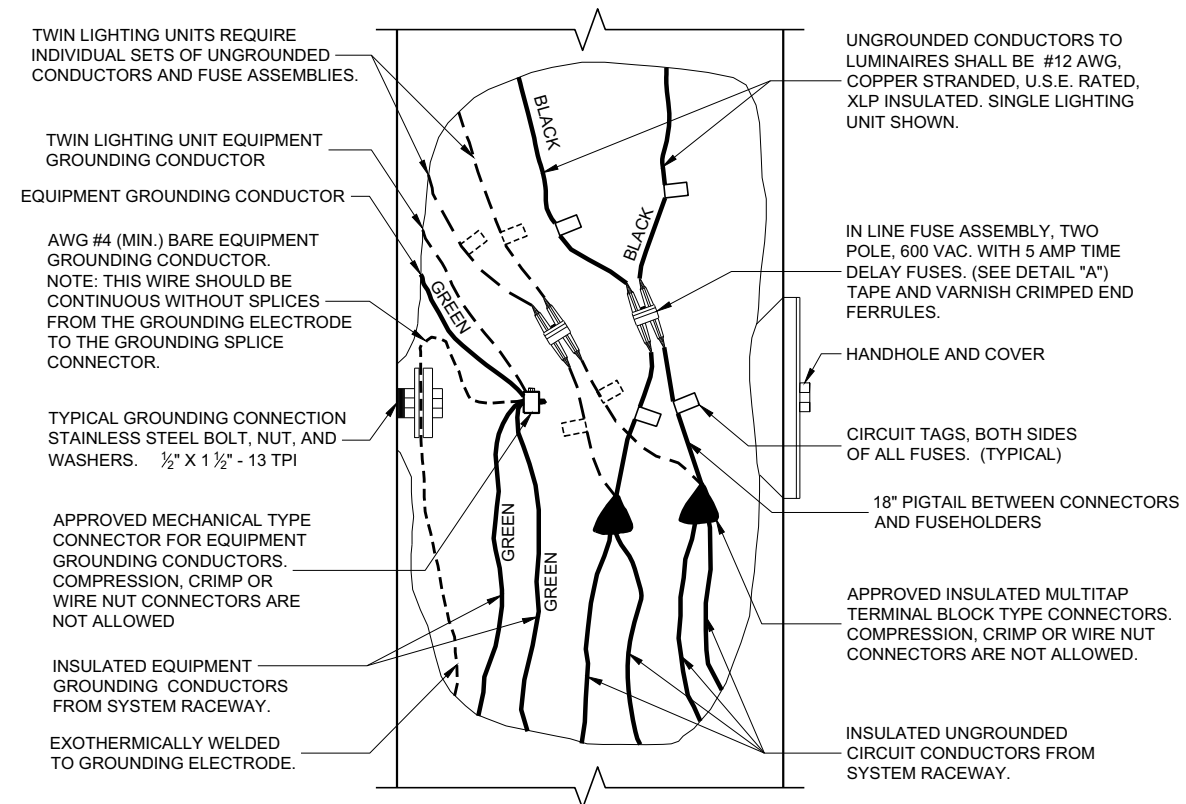


TYPICAL GROUNDING CONNECTIONS
NUT, BOLT AND WASHERS SHALL BE STAINLESS STEEL

LIGHTING UNIT CODE (TYPICAL)



3 WIRE - 120, 240 OR 480 VAC (UNGROUNDING CONDUCTORS) WITH GROUNDING CONDUCTOR AND EQUIPMENT GROUNDING CONDUCTOR



2 WIRE - 240 OR 480 VAC (UNGROUNDING CONDUCTORS) WITH EQUIPMENT GROUNDING CONDUCTOR

NON - FREEWAY LIGHTING UNIT POLE WIRING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2018 /S/ Ahmet Demirelek
DATE STATE ELECTRICAL ENGINEER

FHWA

GENERAL NOTES

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN IN THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

USE THIS DETAIL IN CONJUNCTION WITH THE DETAIL FOR ELECTRICAL HANDHOLE WIRING, SDD10A01.


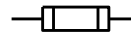

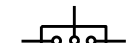





USE TIME DELAY FUSE PER LUMINAIRE MANUFACTURER RECOMMENDATION.

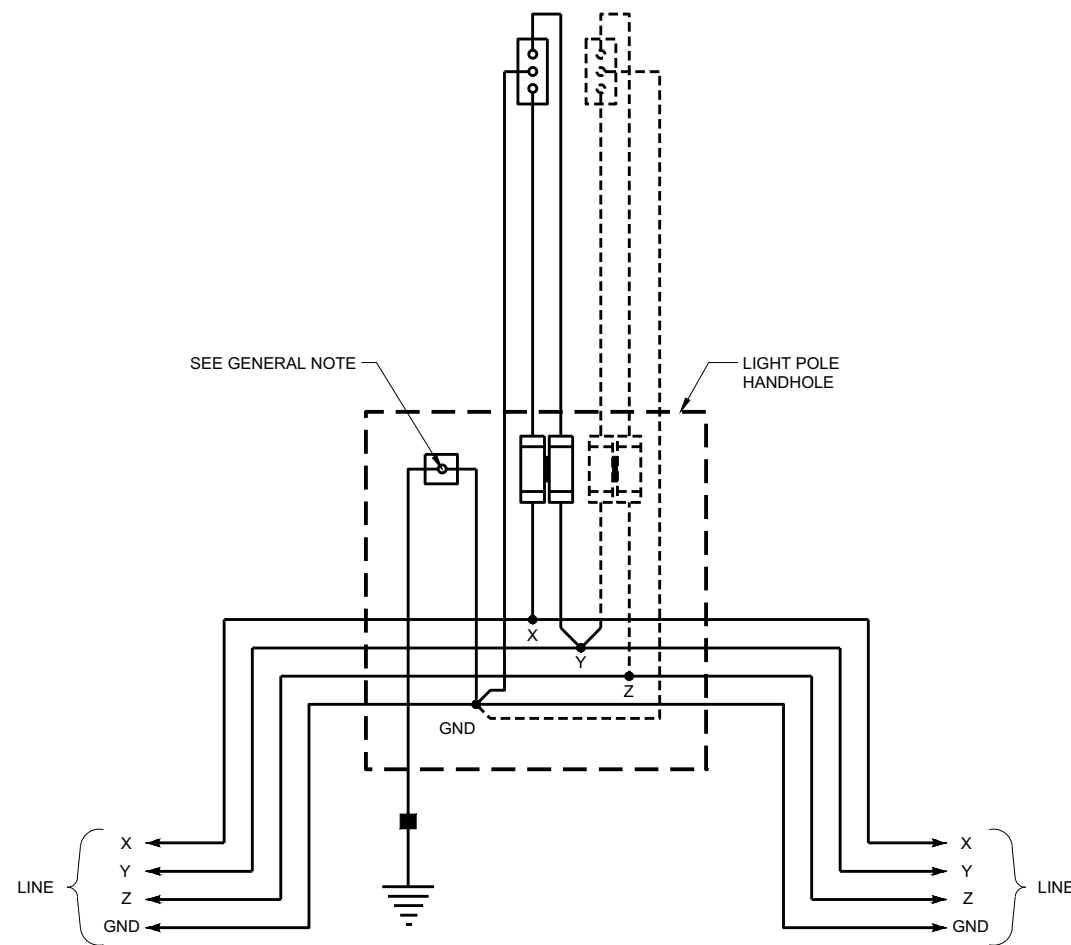
THE GROUNDING ELECTRODE CONDUCTOR SHALL BE CONTINUOUS WITHOUT SPLICES FROM THE GROUNDING ELECTRODE THROUGH THE HANDHOLE GROUNDING LUG TO THE CONNECTOR.

WIRING FOR SINGLE LUMINAIRE POLES IS SHOWN WITH SOLID LINES. WIRING FOR THE SECOND LUMINAIRE OF TWIN LUMINAIRE POLES IS SHOWN WITH DOTTED LINES.

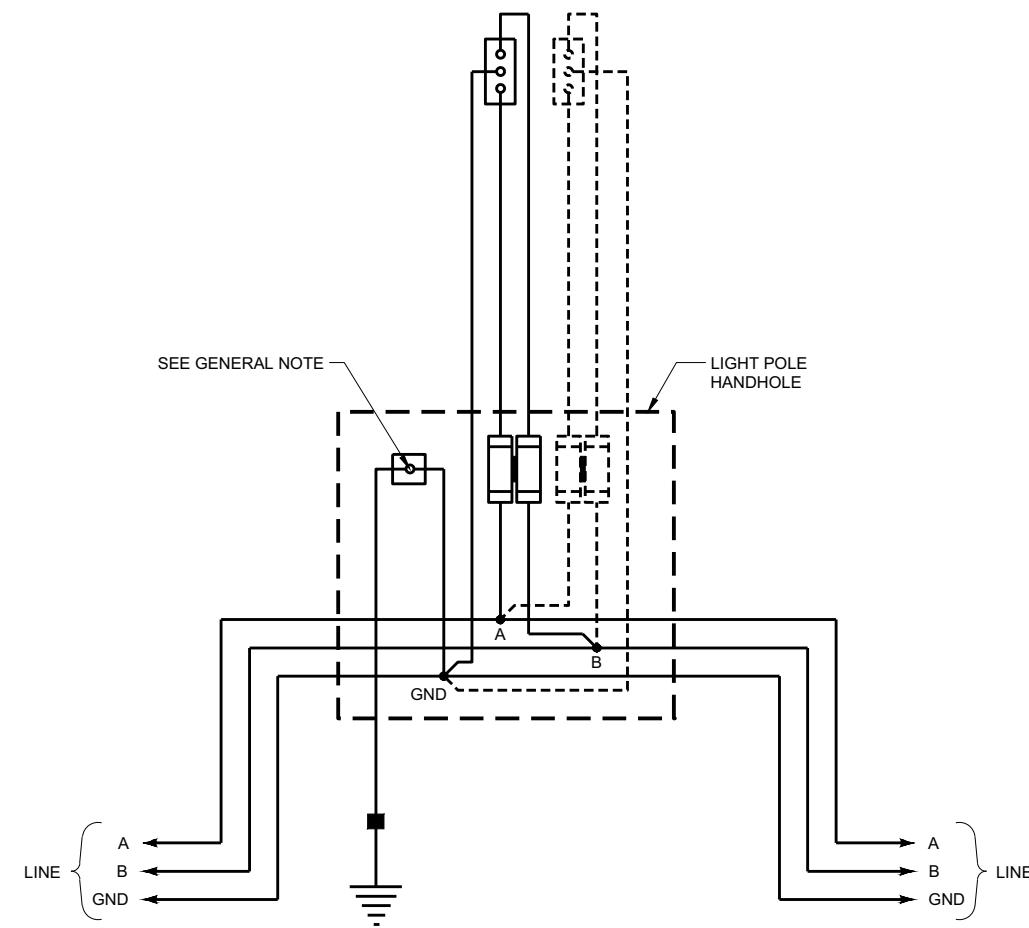
THE PLANS WILL SHOW WHICH CIRCUIT LEG(S) ARE CONNECTED TO EACH INSTALLATION.

LEGEND

| | |
|--|--------------------------------|
| A,B,X,Y,Z | UNGROUNDING CIRCUIT CONDUCTORS |
| N | GROUNDING CIRCUIT CONDUCTORS |
| GND | EQUIPMENT GROUNDING CONDUCTOR |
| P | POLE (ELECTRICAL CIRCUIT) |
| ϕ | PHASE (ELECTRICAL CURRENT) |
|  | HANDHOLE GROUND LUG |
|  | SINGLE-POLE (1P) FUSE ASSEMBLY |
|  | TWO-POLE (2P) FUSE ASSEMBLY |
|  | UNFUSED LUMINAIRE |
|  | EQUIPMENT GROUNDING ELECTRODE |
|  | TERMINAL |
|  | SPLICE |
|  | CONDUCTOR |
|  | EXOTHERMIC WELD |



TYPICAL WIRING DIAGRAM
PHASE - TO - PHASE DELTA SYSTEM
3 - ϕ 480VAC 3 WIRE



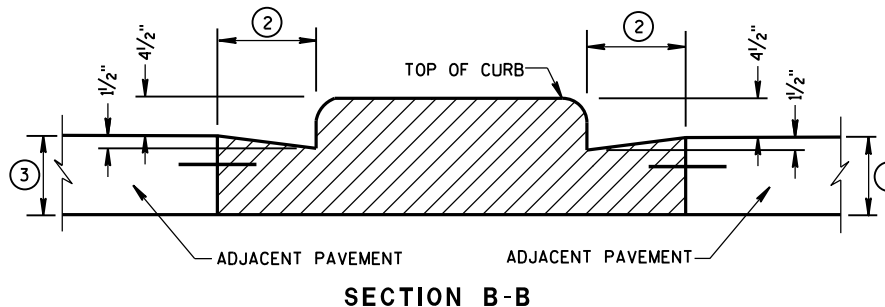
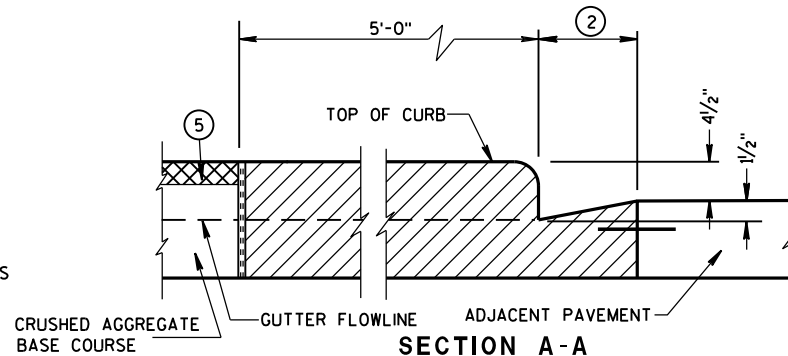
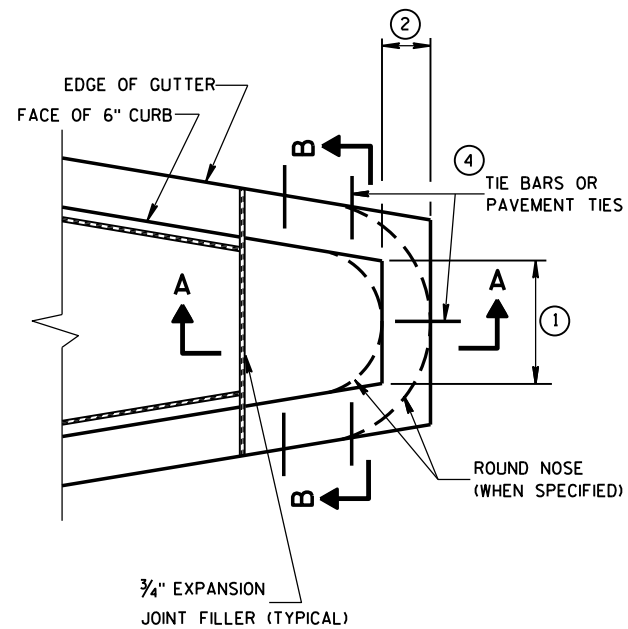
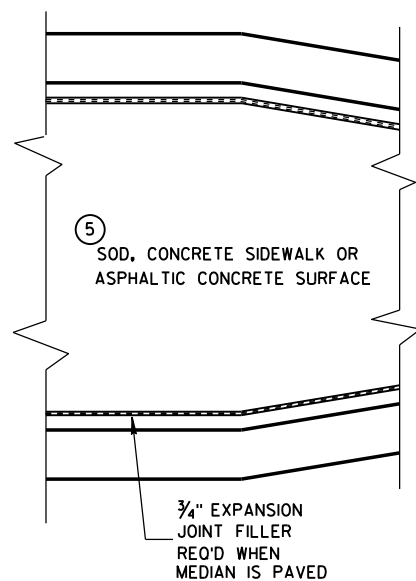
TYPICAL WIRING DIAGRAM
UNGROUNDING SYSTEM
1 - ϕ 120 - 120VAC 2 WIRE

ELECTRICAL DETAILS
GROUND MOUNT LIGHT POLES
PHASE - TO - PHASE SYSTEMS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2018 /S/ Ahmet Demirelek
DATE STATE ELECTRICAL ENGINEER

FHWA

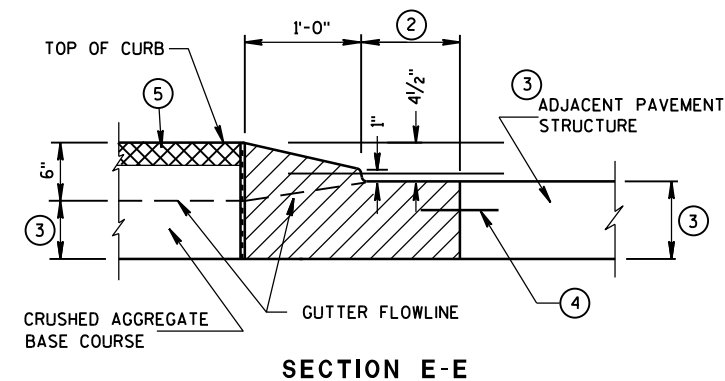
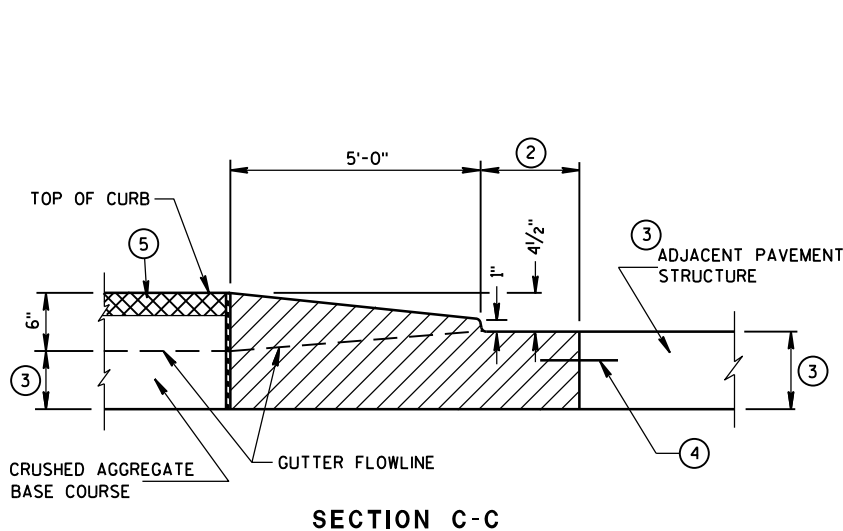
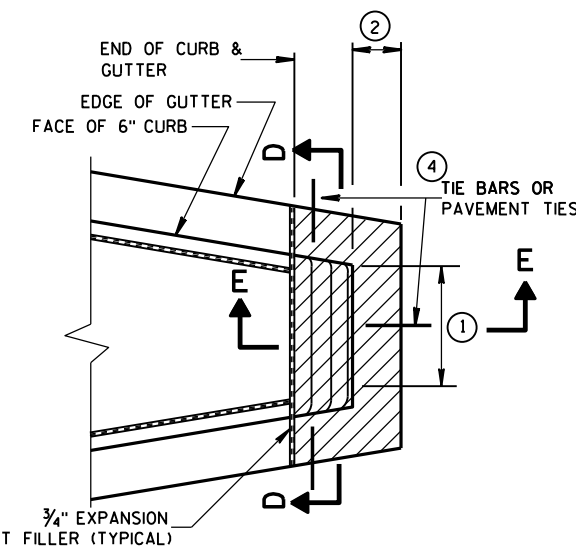


CONCRETE MEDIAN BLUNT NOSE DETAIL

GENERAL NOTES

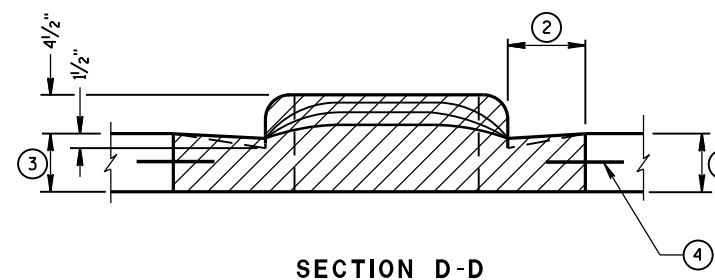
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

- ① SEE PLAN FOR MEDIAN NOSE WIDTH AND RADIUS (FOR ROUND NOSE ALTERNATE).
- ② WIDTH OF GUTTER TO MATCH EXISTING ADJACENT GUTTER OR AS SPECIFIED ELSEWHERE IN THE PLAN.
- ③ DEPTH EQUAL TO ADJACENT PAVEMENT. ADJACENT PAVEMENT STRUCTURE DETAILS ARE SHOWN ON THE PLAN. TYPICAL OPTIONS ARE:
 - (1) NEW OR EXISTING CONCRETE PAVEMENT.
 - (2) ASPHALTIC CONCRETE PAVEMENT OVER NEW OR EXISTING CONCRETE BASE COURSE.
 - (3) ASPHALTIC CONCRETE PAVEMENT OVER CRUSHED AGGREGATE BASE COURSE.
- ④ TIE BARS OR PAVEMENT TIES REQUIRED IN NEW CONCRETE PAVEMENT OR CONCRETE BASE COURSE. TIE BARS SHALL BE NO. 4 X 2'-0" SPACED AT 2'-0" C-C.
- PAVEMENT TIES REQUIRED IN EXISTING CONCRETE BASE COURSE. PAVEMENT TIES SHALL BE NO. 6 X 1'-0" SPACED AT 3'-0" C-C INSTALLED ON A HORIZONTAL SKEW OF 6:1. THE DIRECTION OF SKEW SHALL ALTERNATE AFTER EVERY ONE OR TWO BARS.
- ⑤ SURFACE TYPE AND DETAILS ARE SHOWN ELSEWHERE IN THE PLAN.



CONCRETE MEDIAN SLOPED NOSE TYPE 2

CONCRETE MEDIAN SLOPED NOSE TYPE 1



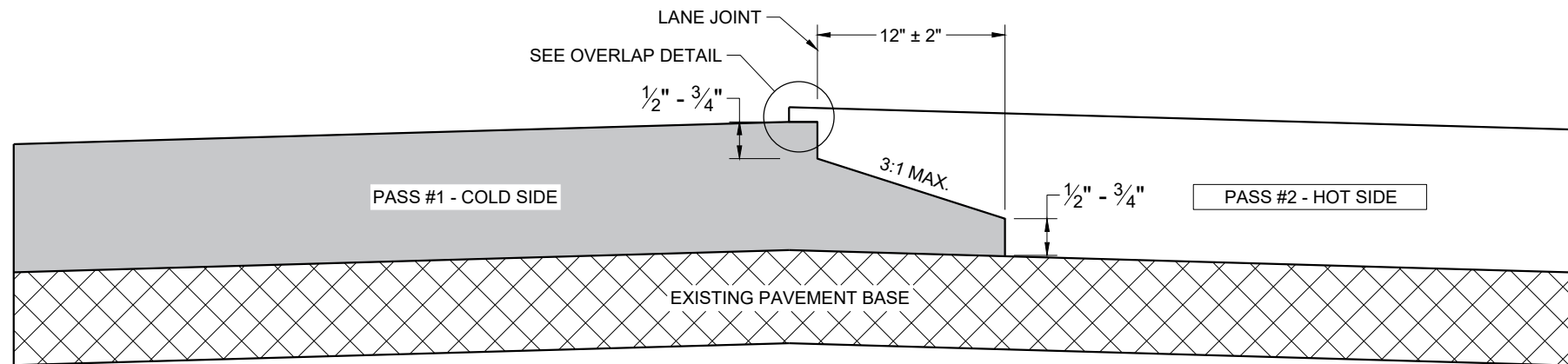
| | |
|--|--|
| CONCRETE MEDIAN NOSE | |
| STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION | |
| APPROVED 6/8/2006 DATE | /s/ Jerry H. Zogg ROADWAY STANDARDS DEVELOPMENT ENGINEER |
| FHWA | |

6

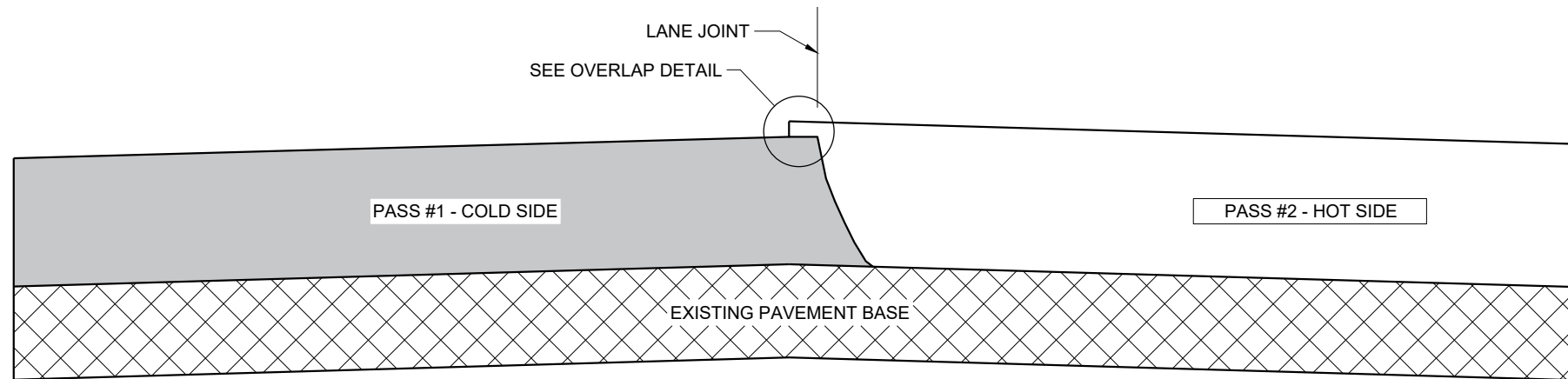
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S.D.D. 11 B 2-2

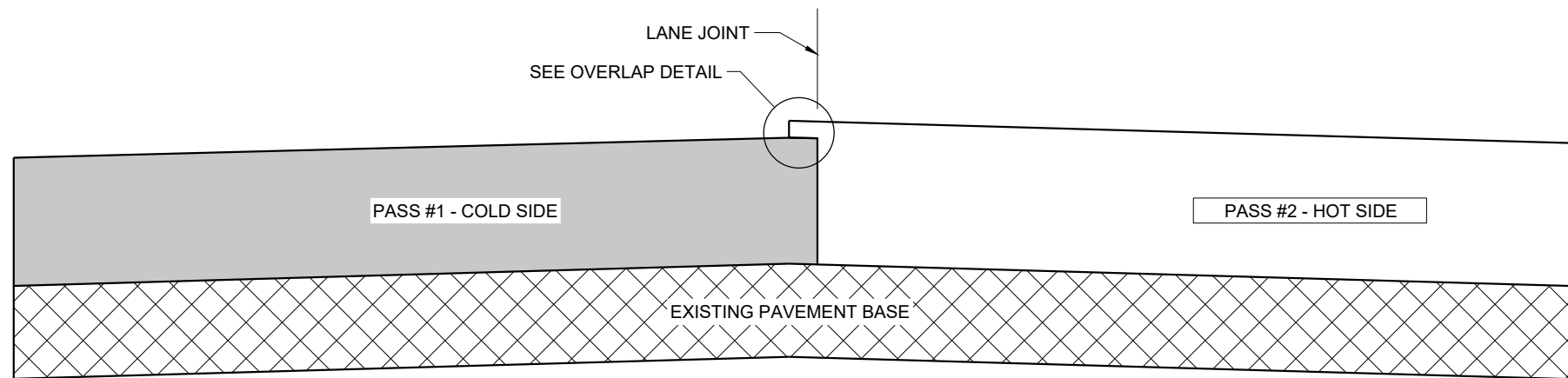
S.D.D. 11 B 2-2



**TYPICAL PAVEMENT CROSS SECTION
NOTCHED WEDGE JOINT**



**TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT**



**TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT (MILLED)**

GENERAL NOTES

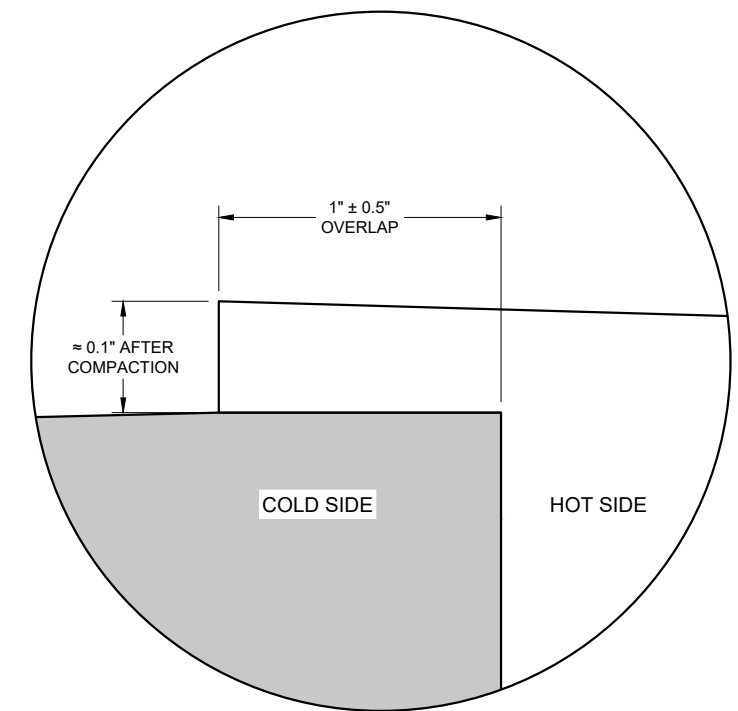
IN ADDITION TO THE DETAILS PROVIDED IN THIS DRAWING, CONFORM TO STANDARD SPECIFICATION 450.3.2.8 FOR WHEN A NOTCHED WEDGE JOINT IS REQUIRED AND FOR GENERAL JOINT CONSTRUCTION REQUIREMENTS.

FOR ALL LONGITUDINAL JOINTS, ENSURE THE PAVER SCREED OVERLAPS THE PREVIOUSLY PLACED PAVEMENT BY $1" \pm 0.5"$ AND THE HOT SIDE OF THE JOINT REMAINS HIGHER THAN THE COLD SIDE BY APPROXIMATELY 0.1" AFTER FINAL COMPACTION. (IT WILL BE FLUSH WHEN PAVING IN ECHELON.)

ONLY REMOVE THE LONGITUDINAL NOTCHED WEDGE JOINT FOR SMA PAVEMENT OR AS DIRECTED BY THE ENGINEER TO ADDRESS SPECIFIC LENGTHS OF JOINT DAMAGED BY TRAFFIC.

WHEN MILLING BACK OR REMOVING ANY LONGITUDINAL JOINT, LIMIT THE MATERIAL REMOVED TO 2" FROM THE TOP NOTCH OR FROM THE VERTICAL JOINT EDGE ON THE COLD SIDE OF THE JOINT.

USE LONGITUDINAL MILLED JOINT AS PLANS SHOW OR THE AS THE ENGINEER DIRECTS.



OVERLAP DETAIL (TYPICAL)

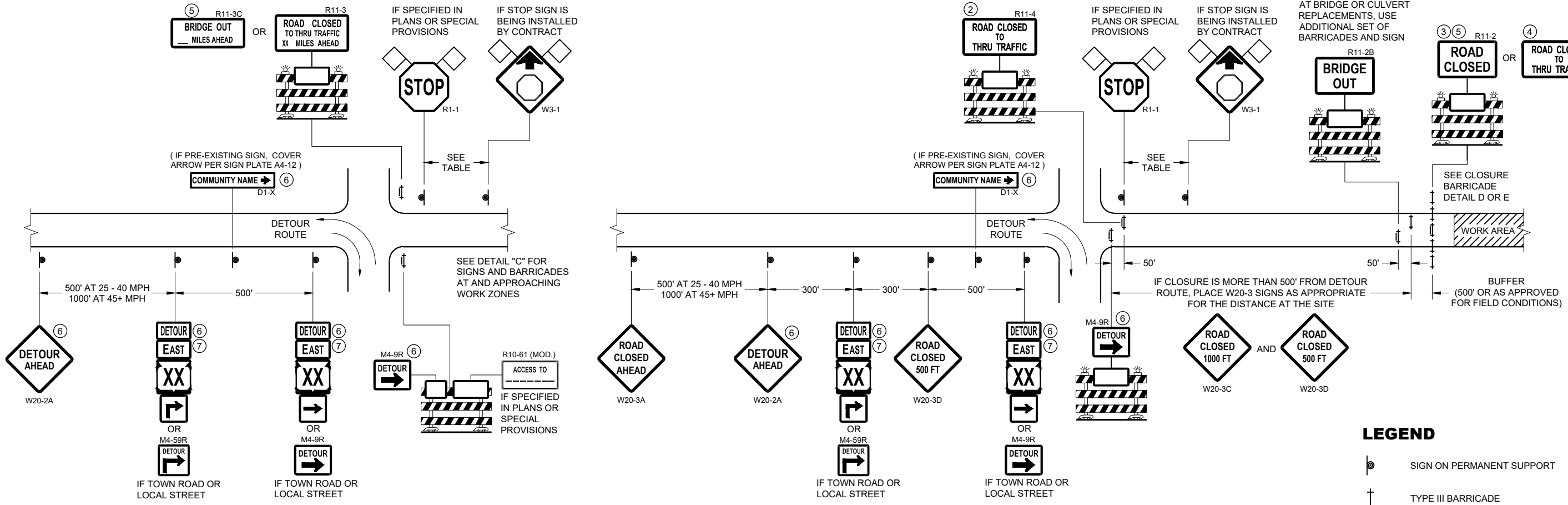
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SDD 13C19 - 03

SDD 13C19 - 03

| | |
|--|---|
| HMA LONGITUDINAL JOINTS | |
| STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION | |
| APPROVED November 2020 DATE | /S/ Steven Hefel HMA PAVEMENT ENGINEER |
| FHWA | |



DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR

WORK ZONE GREATER THAN OR EQUAL TO 1/2 MILE FROM
 DETOUR ROUTE (1000 FEET IF URBAN)

DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR

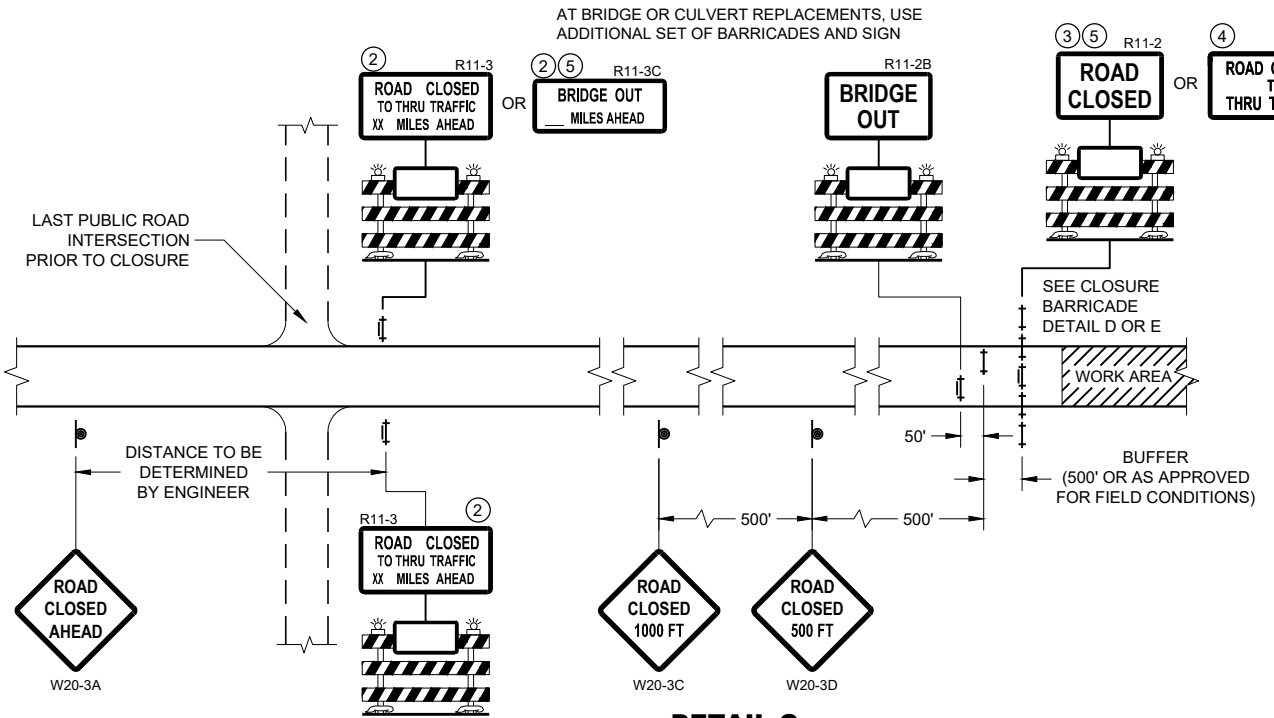
WORK ZONE LESS THAN 1/2 MILE FROM
 DETOUR ROUTE (1000 FEET IF URBAN)

- LEGEND**
- SIGN ON PERMANENT SUPPORT
 - TYPE III BARRICADE
 - TYPE III BARRICADE WITH ATTACHED SIGN
 - TYPE "A" WARNING LIGHT (FLASHING)
 - WORK AREA
 - FLAGS, 16" X 16" MIN. (ORANGE)

- M4 - 8
- M3 - X
- M1 - 4
- M1 - 6
- M1 - 5A
- M05 - 1
- M06 - 1

| SPEED LIMIT (MPH) | "STOP AHEAD" ADVANCE WARNING DISTANCE (FT) |
|-------------------|--|
| 25 | 200 |
| 30 | 200 |
| 35 | 350 |
| 40 | 350 |
| 45 | 500 |
| 50 | 550 |
| 55 | 750 |

SEE SDD 15C2-SHEET "b"
 FOR GENERAL NOTES
 AND FOOTNOTES ① THROUGH ⑦

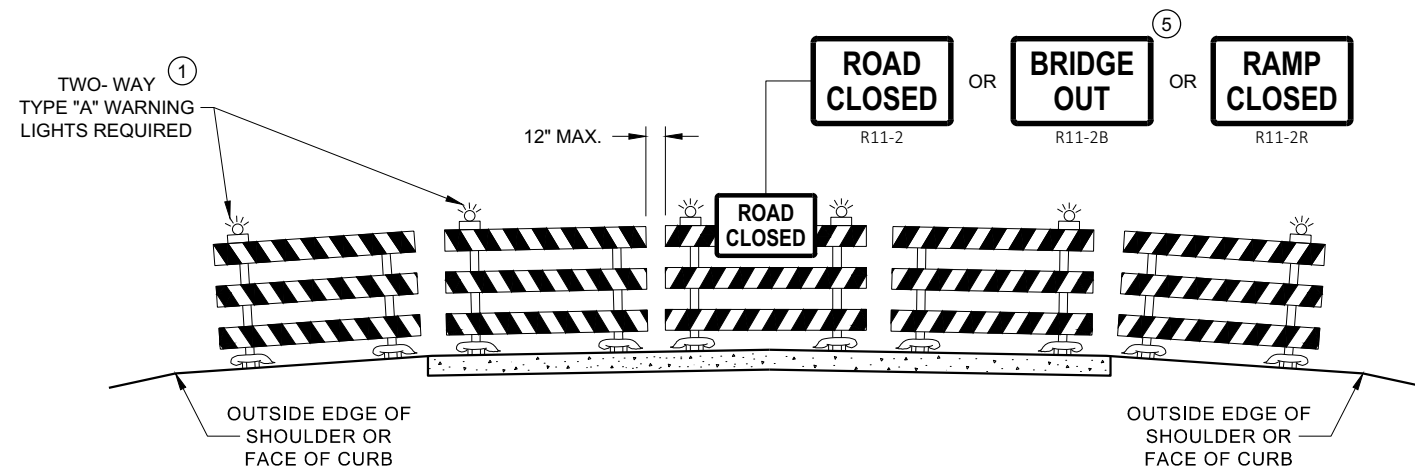


DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR

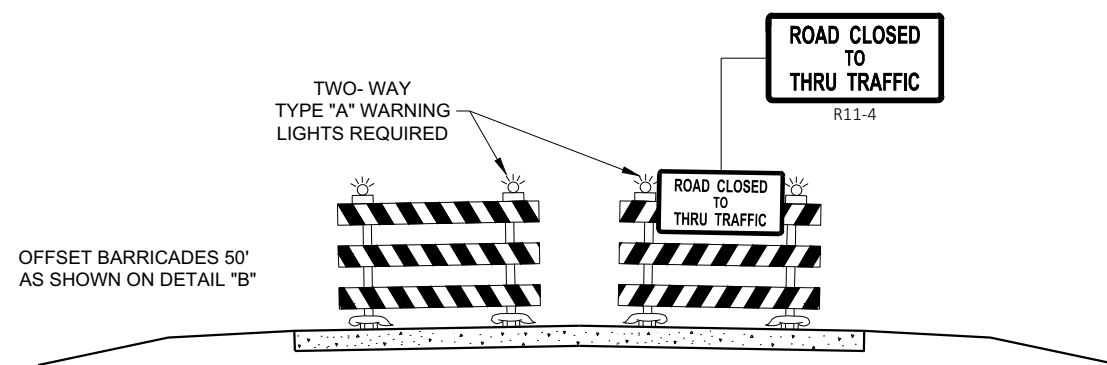
**BARRICADES AND SIGNS
 FOR MAINLINE CLOSURES**

STATE OF WISCONSIN
 DEPARTMENT OF TRANSPORTATION

APPROVED
 February 2020 /S/ Andrew Heidtke
 DATE DATE WORK ZONE ENGINEER
 FHWA



**DETAIL D
ROAD CLOSURE BARRICADE DETAIL
APPROACH VIEW**



**DETAIL E
LANE CLOSURE BARRICADE DETAIL
APPROACH VIEW**

SEE SDD 15C2 - SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW - INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11 - 2, R11 - 3, M4 - 9, R11 - 4, AND R10 - 61 SIGNS PLACED ON THE BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE RAIL OR BOTTOM RAILS.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

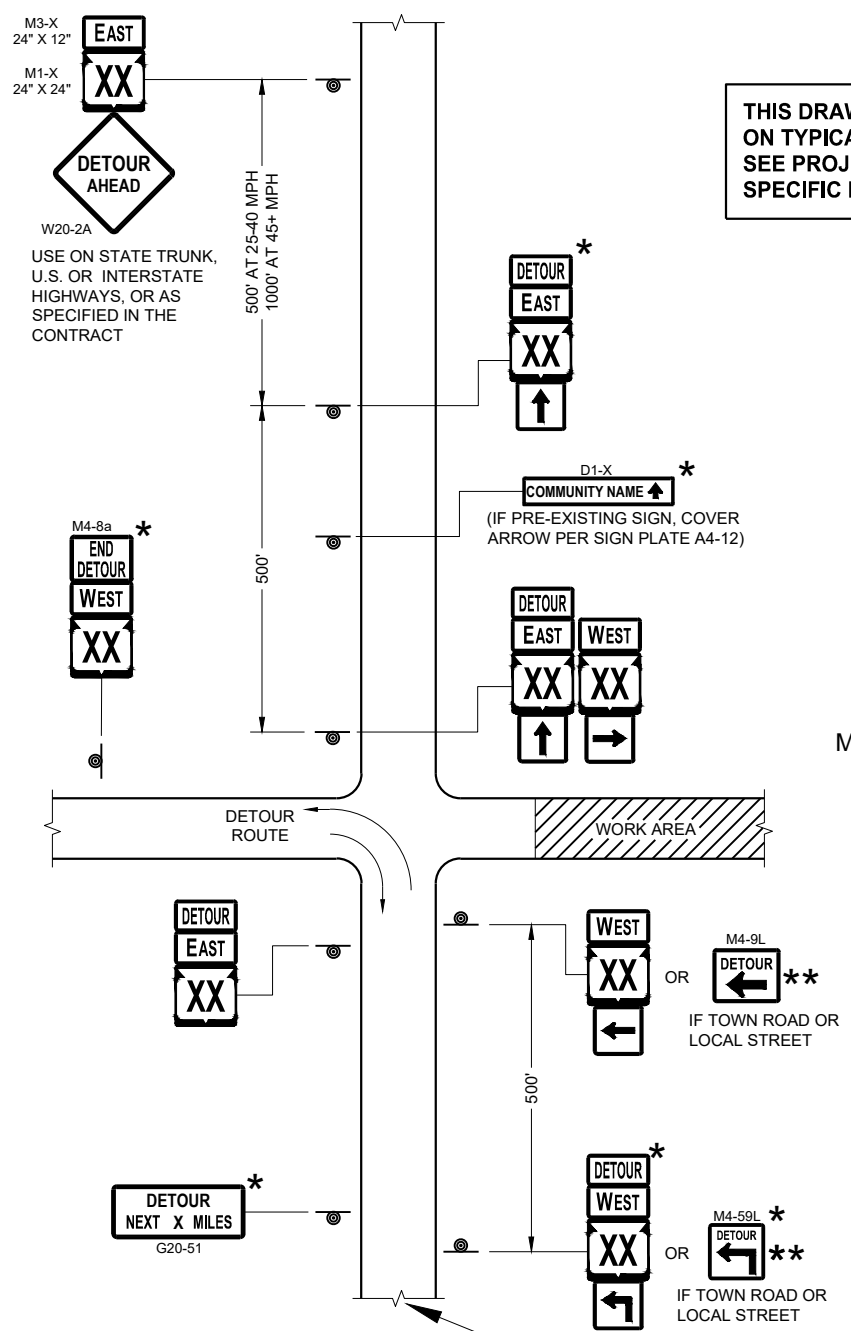
- R11 - 2 SHALL BE 48" X 30"
- R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60" X 30"
- M4 - 9 SHALL BE 30" X 24"
- M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1 - 1 SHALL BE 36" X 36"

- ① TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT SPACING).
- ② THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.
- ③ FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".
- ④ FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".
- ⑤ FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.
- ⑥ INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.
- ⑦ "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

**BARRICADES AND SIGNS
FOR
VARIOUS CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER



SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS AND DETAIL A OR B ON SDD SHEET 15C02 - SHEET "a"

THIS DRAWING PROVIDES GENERAL GUIDANCE ON TYPICAL DETOUR SIGN LAYOUT AND SPACING. SEE PROJECT DETOUR SIGNING SHEETS FOR SPECIFIC DETAILS FOR EACH PROJECT.

LEGEND

- SIGN ON PERMANENT SUPPORT
- WORK AREA
- M4 - 8
- M3 - X
- M1 - 4
- M1 - 6
- M1 - 5A
- M05 - 1
- M06 - 1
- M06 - 1

GENERAL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. MODIFY EXISTING SIGNS WHERE POSSIBLE.

THE SPACING BETWEEN TRAFFIC CONTROL AND DETOUR SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

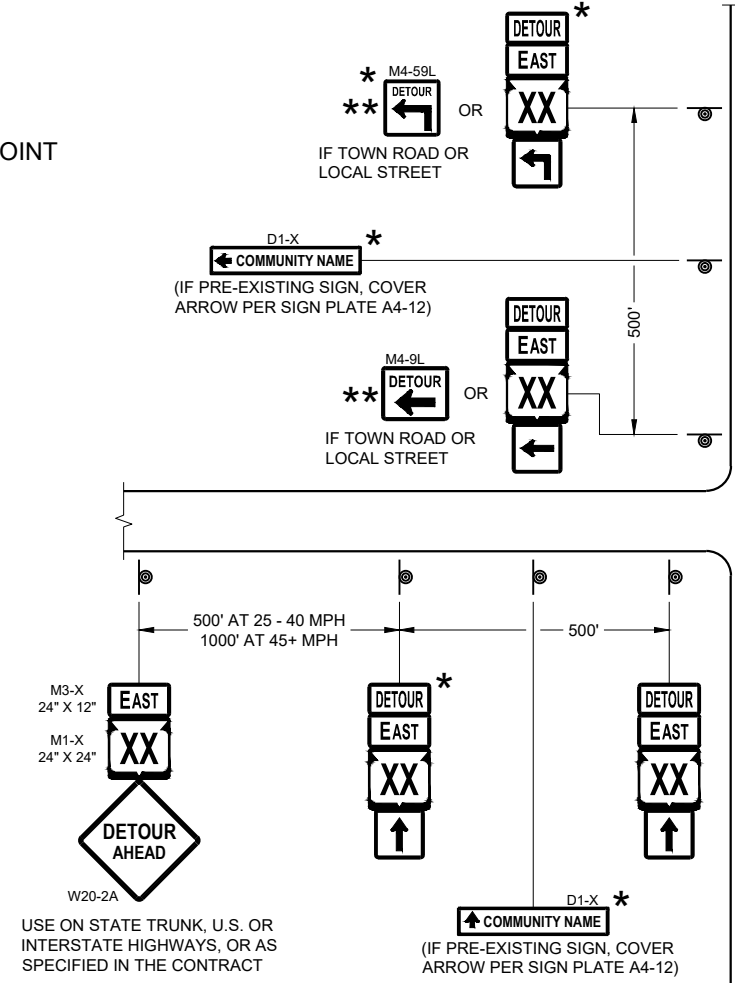
"MO" SIGNS ARE THE SAME AS "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGN SIZES SHALL BE AS FOLLOWS:

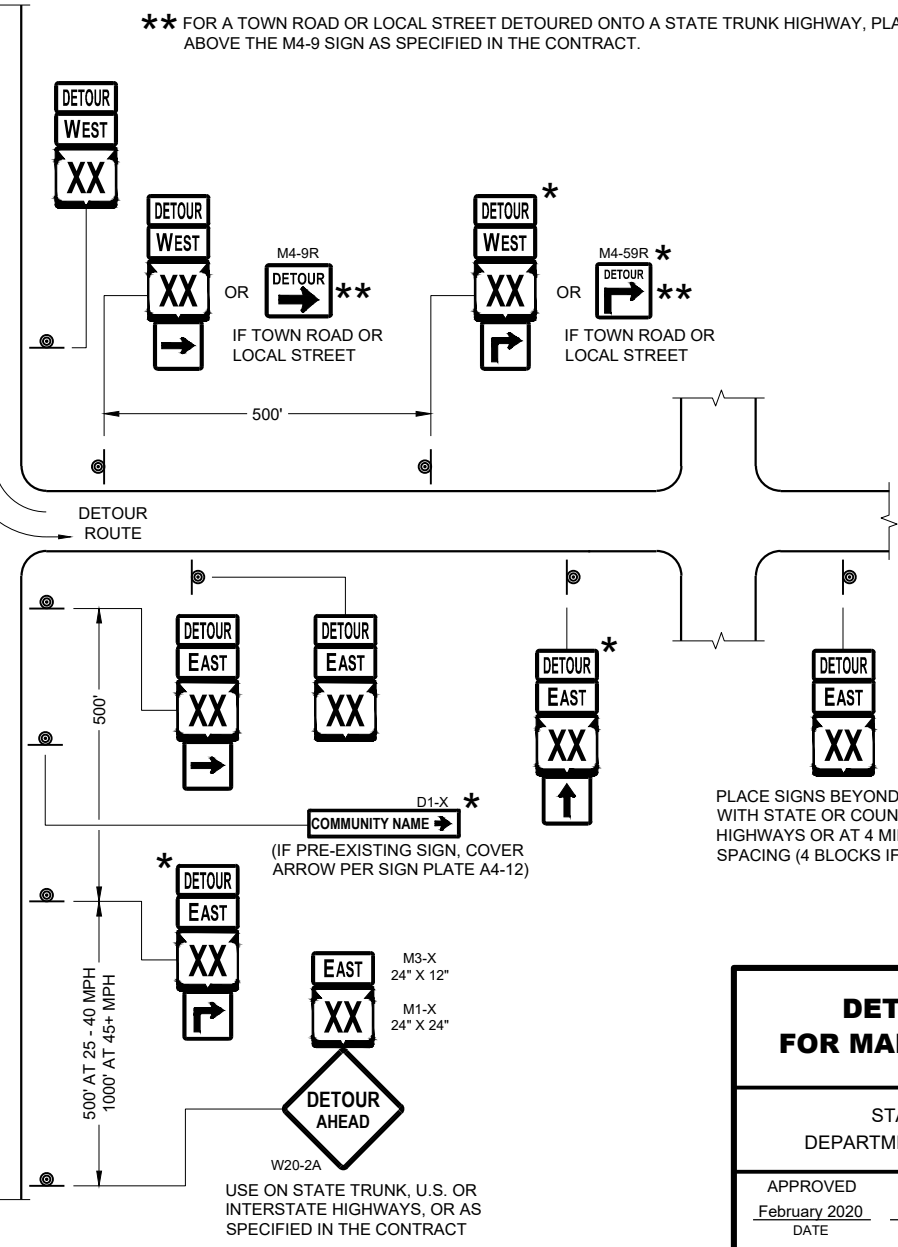
- M3-X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4-8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1-4, M1-5A AND M1-6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- M05-1 AND M06-1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- M4-9 AND M4-59 SHALL BE 30" X 24"
- M4-8a SHALL BE 24" X 18"
- G20-51 SHALL BE 60" X 24"
- W20-2A SHALL BE 48" X 48"
- D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.

- * OPTIONAL SIGNS. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS.
- ** FOR A TOWN ROAD OR LOCAL STREET DETOURED ONTO A STATE TRUNK HIGHWAY, PLACE A ROAD NAME PLAQUE ABOVE THE M4-9 SIGN AS SPECIFIED IN THE CONTRACT.

MATCH POINT



**DETAIL F
DETOUR SIGNING**



**DETOUR SIGNING
FOR MAINLINE CLOSURES**

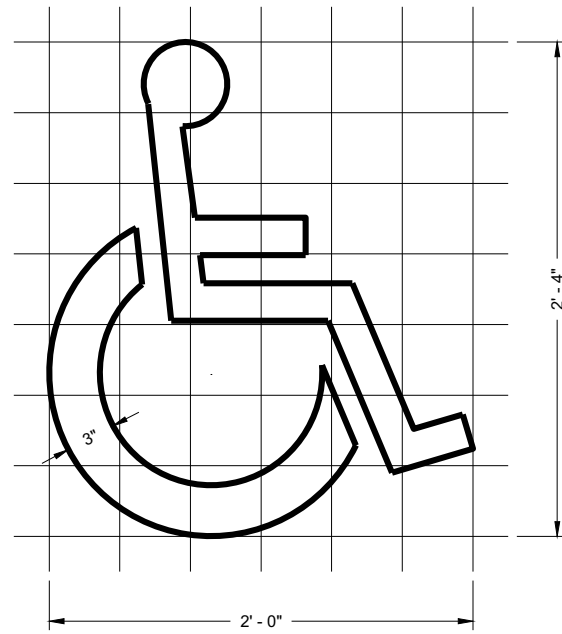
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

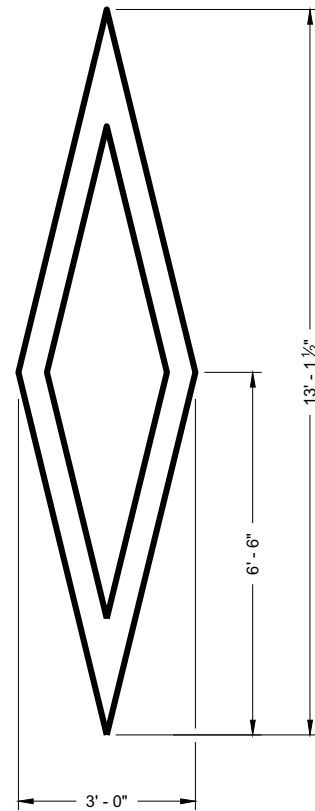
FHWA

GENERAL NOTES

DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.



HANDICAP SYMBOL

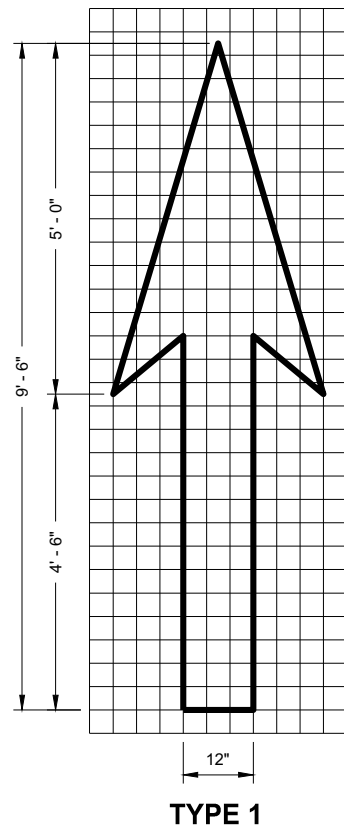


**PREFERENTIAL
LANE SYMBOL**

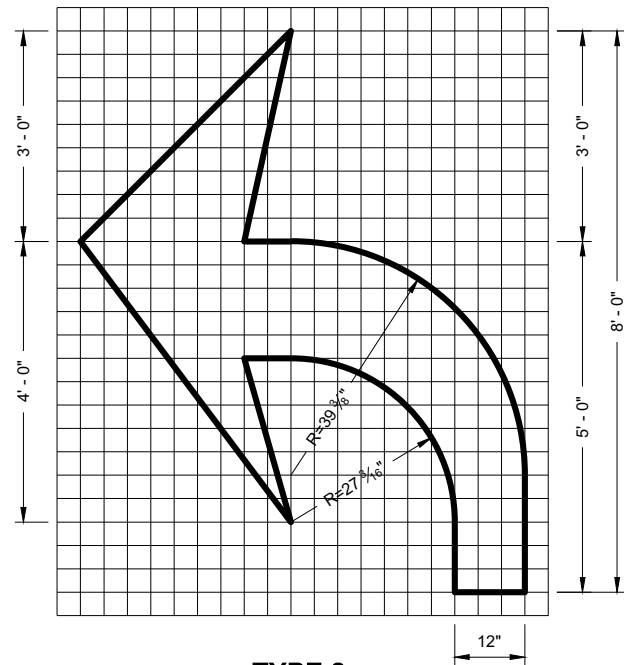
PAVEMENT MARKING SYMBOLS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

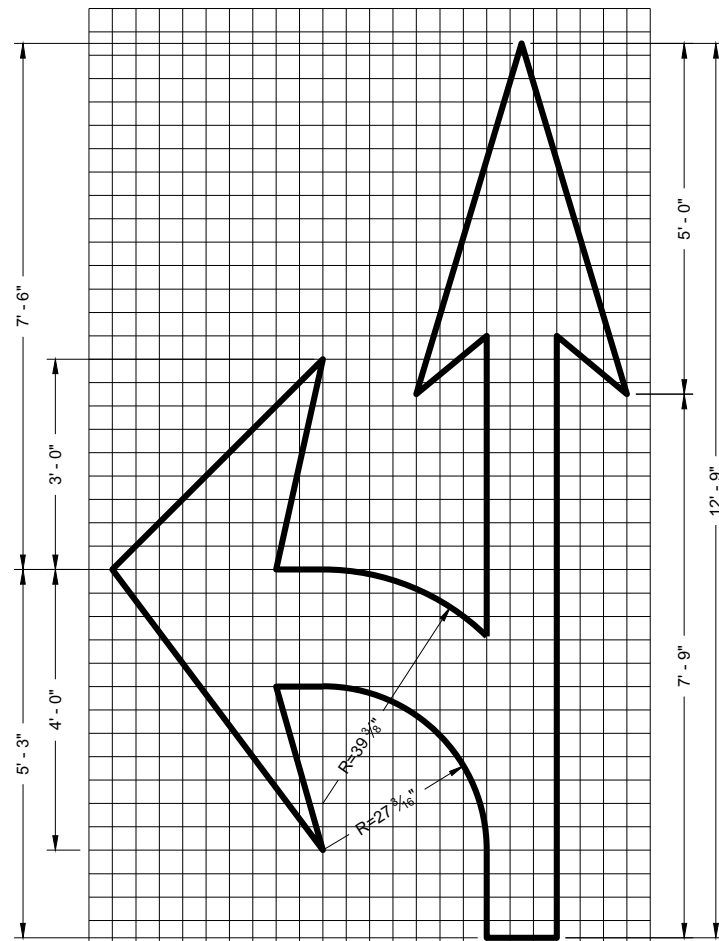
APPROVED
November 2019 /S/ Matthew Rauch
DATE STATE SIGNING AND MARKING
ENGINEER



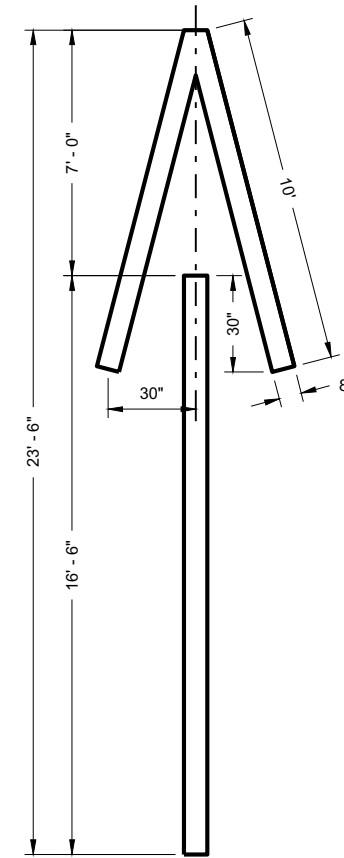
TYPE 1



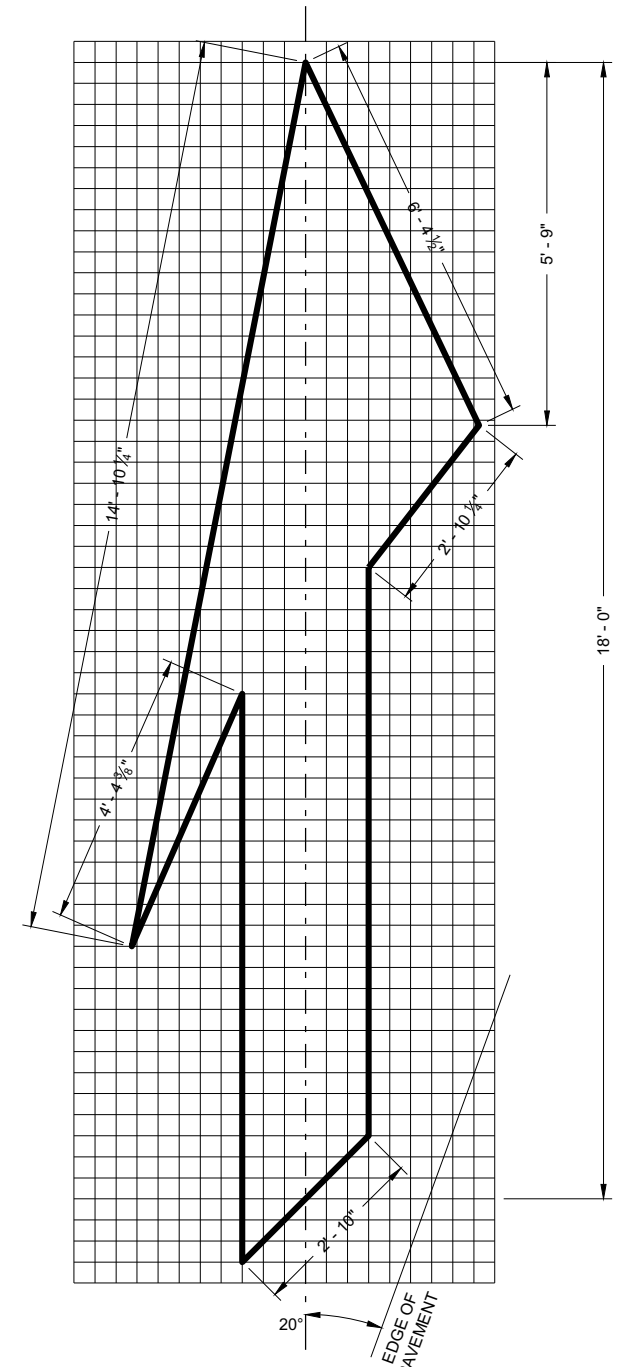
TYPE 2



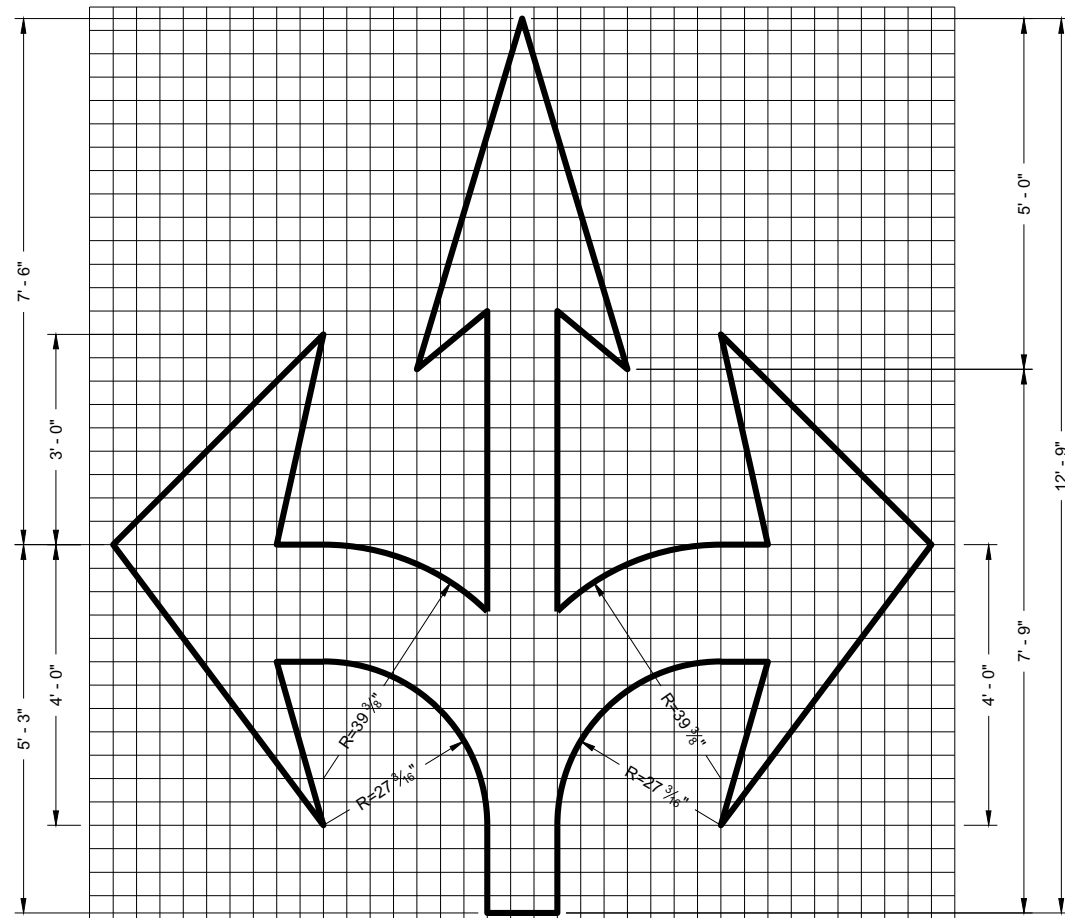
TYPE 3



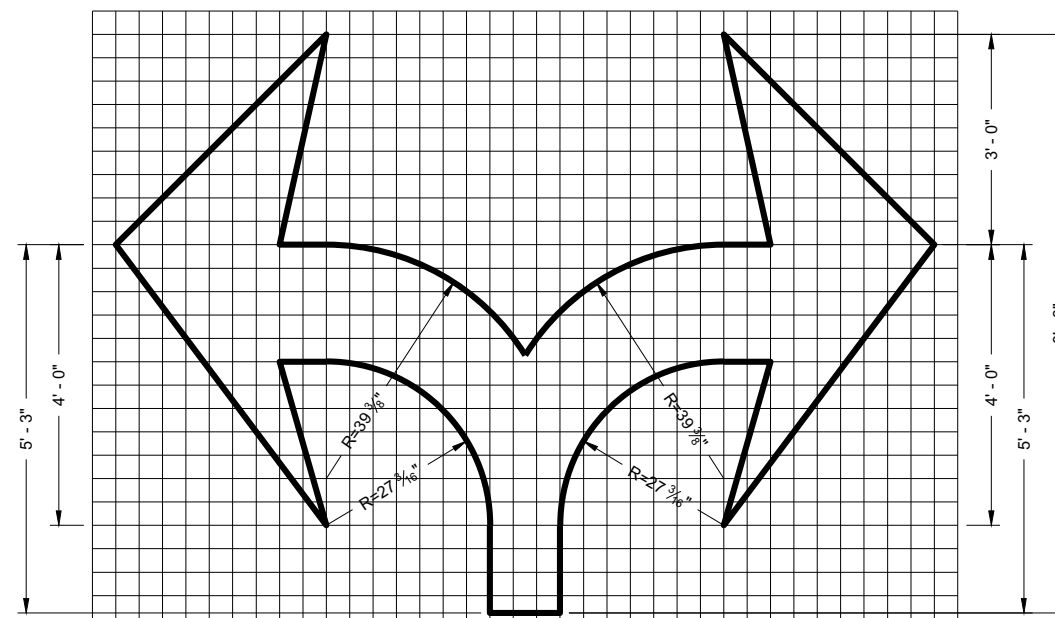
TYPE 4



TYPE 5 LANE DROP ARROW



TYPE 6



TYPE 7

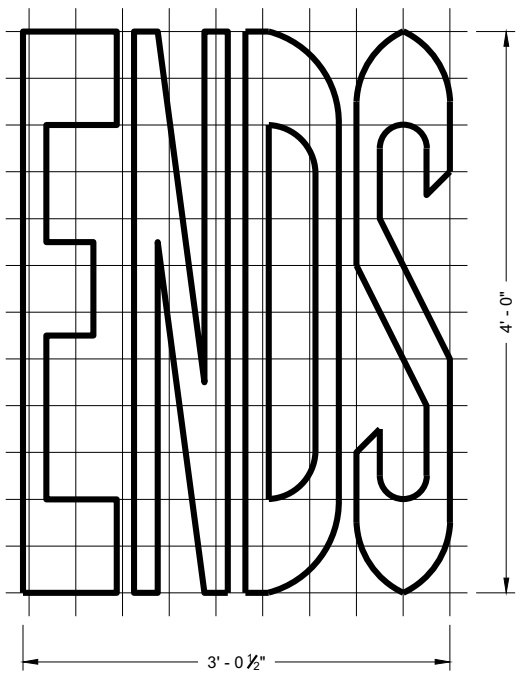
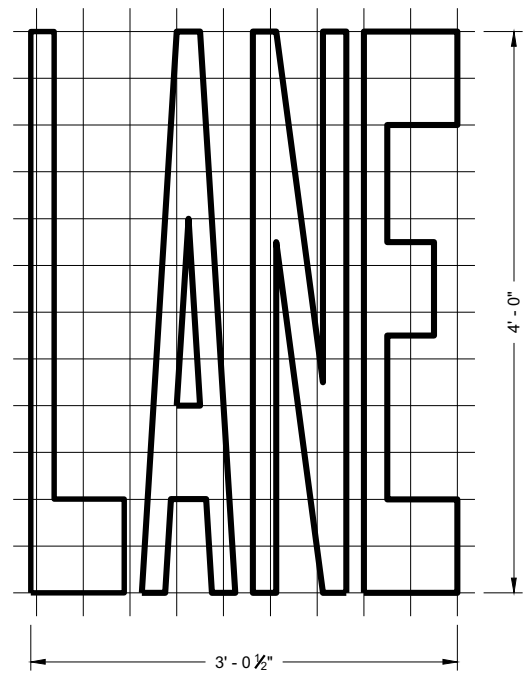
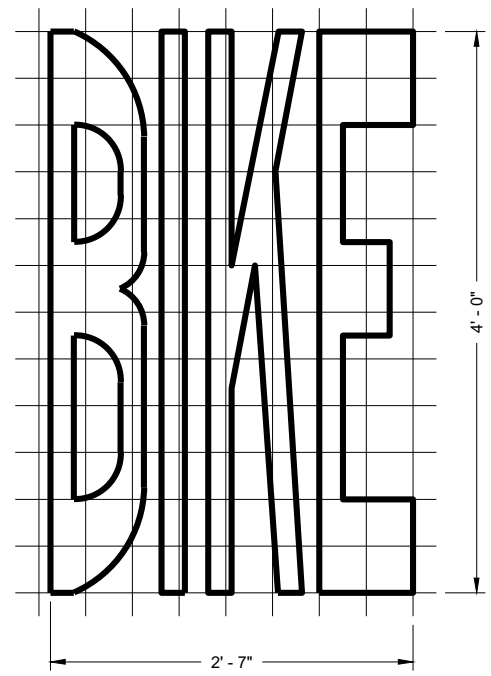
GENERAL NOTES

DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

PAVEMENT MARKING ARROWS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

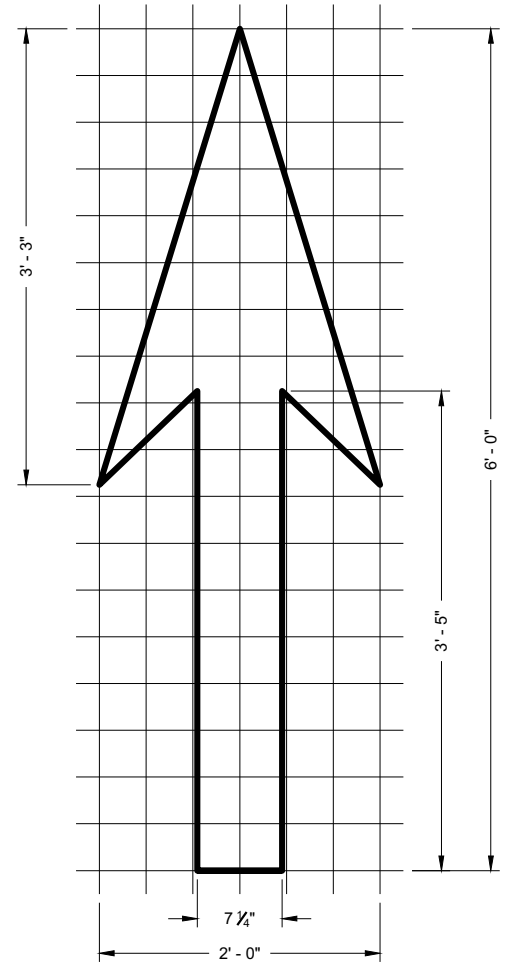
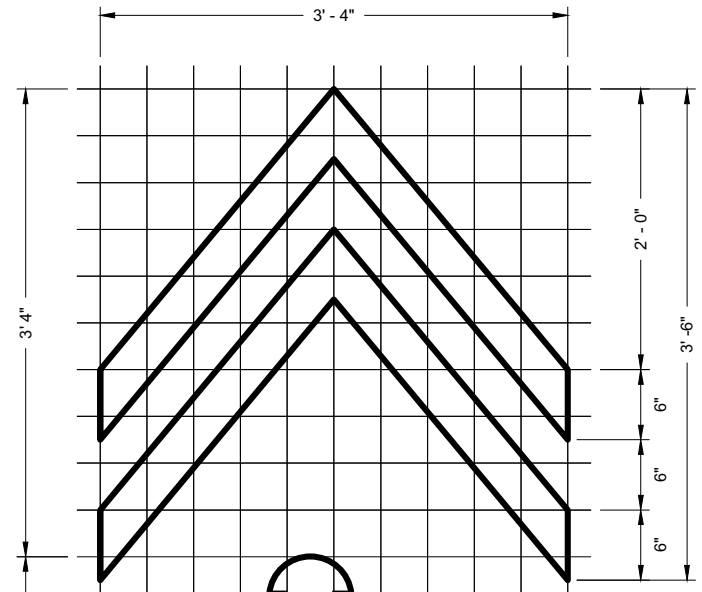
APPROVED
November 2019
DATE /S/ Matthew Rauch
STATE SIGNING AND MARKING
ENGINEER
FHWA



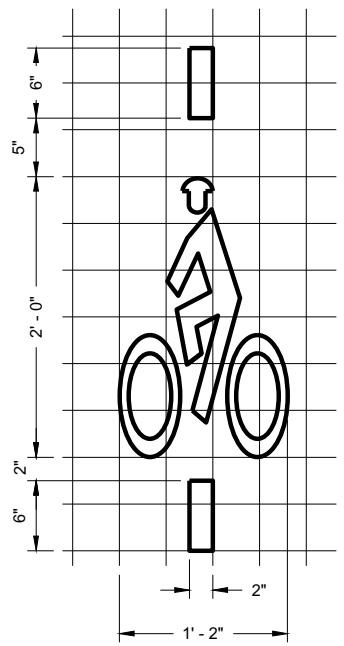
BIKE LANE WORDS

GENERAL NOTES

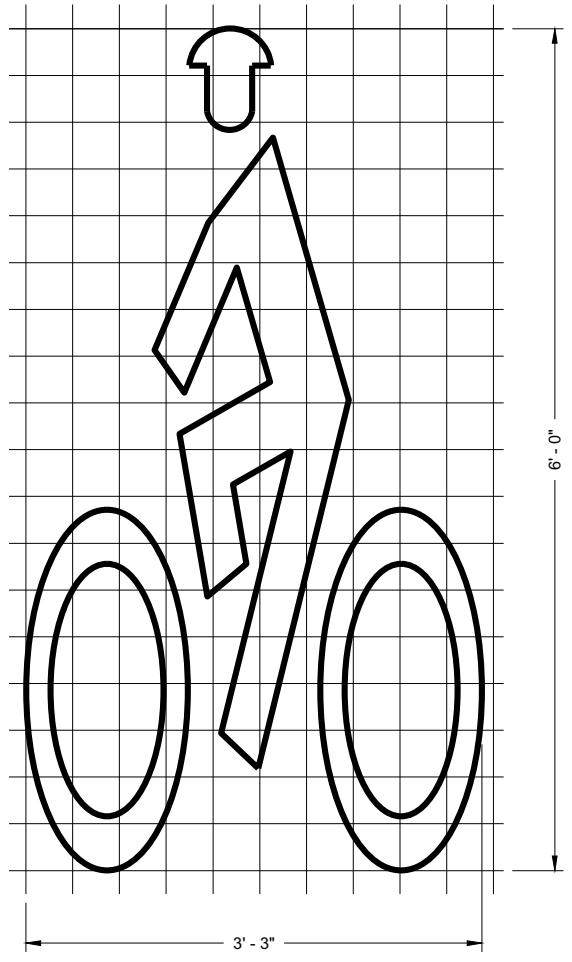
DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.



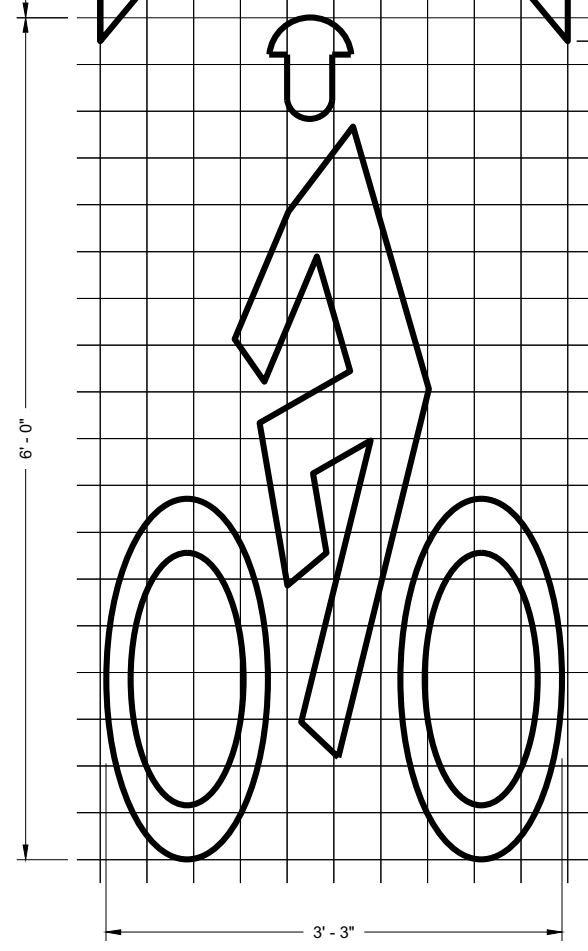
BIKE LANE ARROW



BIKE DETECTOR PAVEMENT MARKING



BIKE LANE SYMBOL



BIKE LANE SYMBOL FOR SHARED LANE

6

6




| | |
|--|--|
| PAVEMENT MARKING FOR BIKE LANES | |
| STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION | |
| APPROVED November 2019 DATE | /s/ Matthew Rauch STATE SIGNING AND MARKING ENGINEER |
| FHWA | |

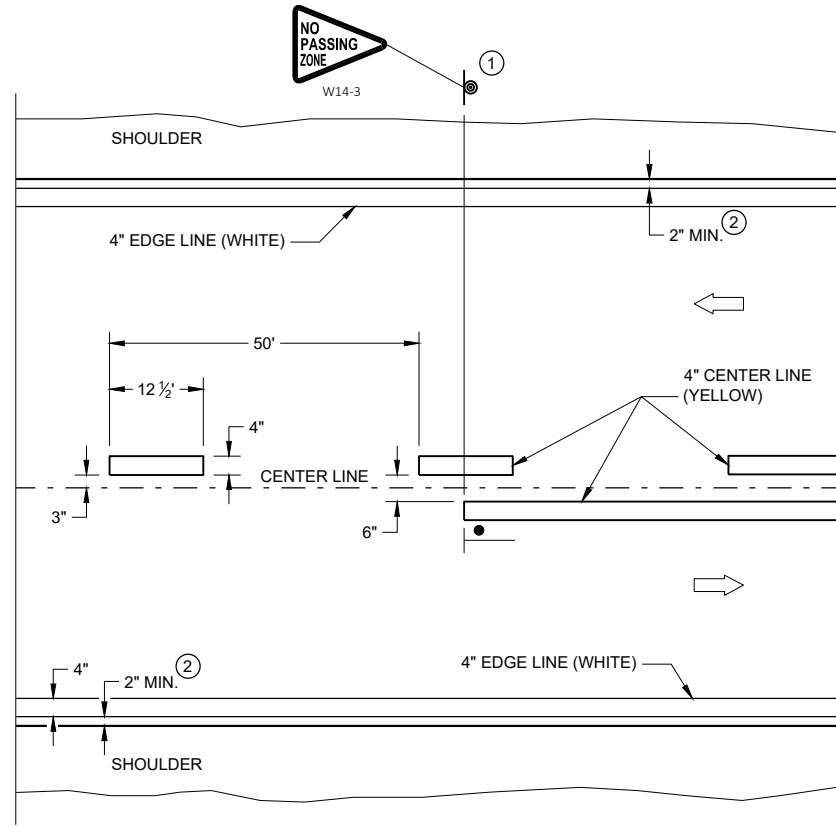
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

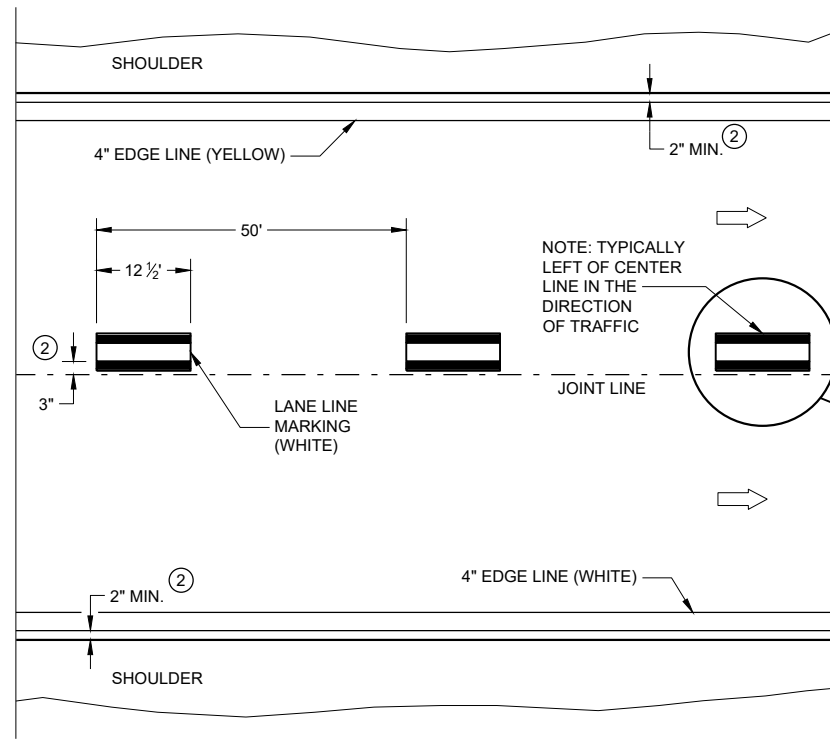
- ① LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING
- ② MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

LEGEND

-  "T" MARKING
-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC

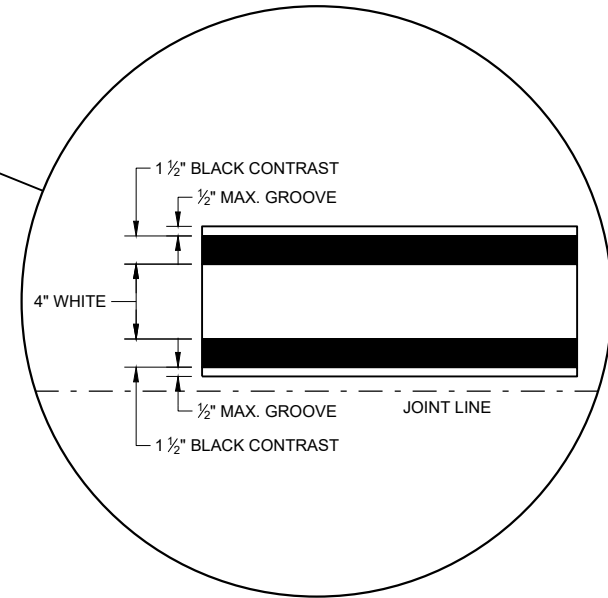


TWO WAY TRAFFIC



ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING



6

6

PERMANENT LONGITUDINAL PAVEMENT MARKINGS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE: May 2022 /S/ Jeannie Silver
STATEWIDE SIGNING AND MARKING ENGINEER

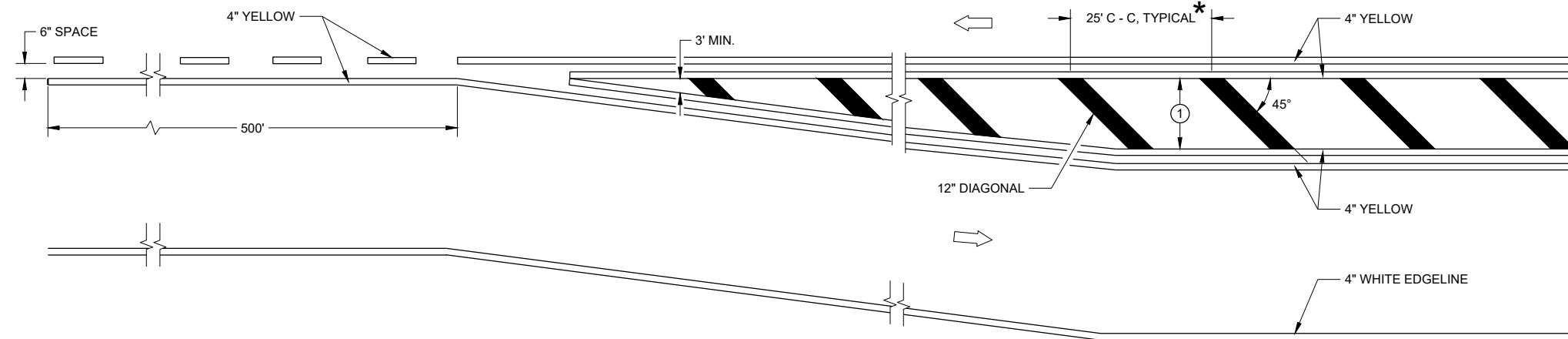
FHWA

GENERAL NOTES

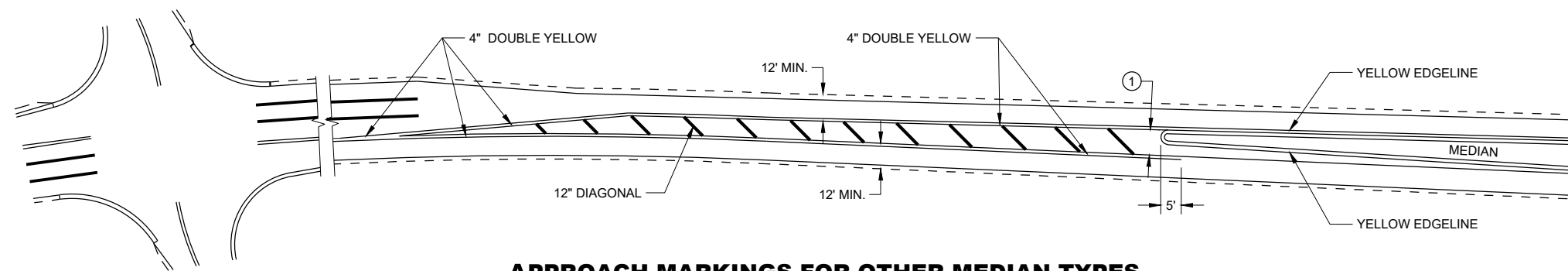
- ① DIAGONALS ARE OPTIONAL WHEN PAINTED ISLAND IS LESS THAN 6 FEET AT THE WIDEST POINT. OMIT DIAGONALS IF WIDTH IS LESS THAN 4 FEET.

➡ DIRECTION OF TRAVEL

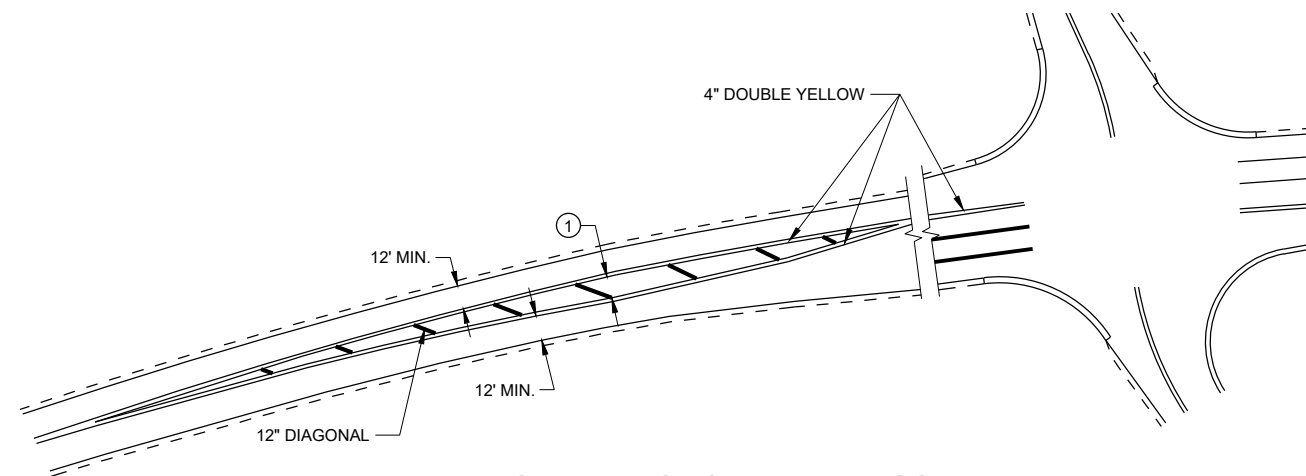
* WHEN THE PAINTED MEDIAN LENGTH IS LESS THAN 50 FEET THE SPACING IS 10'.



MEDIAN ISLAND DETAIL



APPROACH MARKINGS FOR OTHER MEDIAN TYPES



NON-APPROACH MARKINGS

6

6

SDD 15C18 - 06a

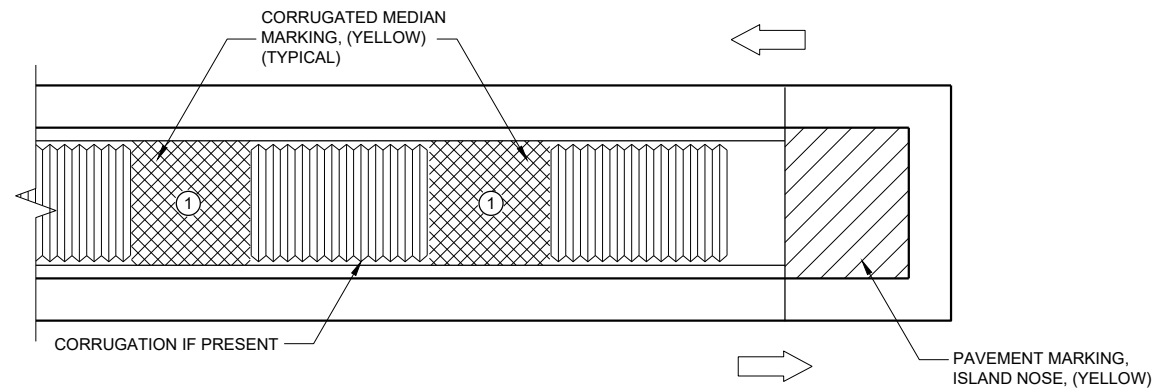
SDD 15C18 - 06a

**MEDIAN ISLAND
PAVEMENT MARKINGS**

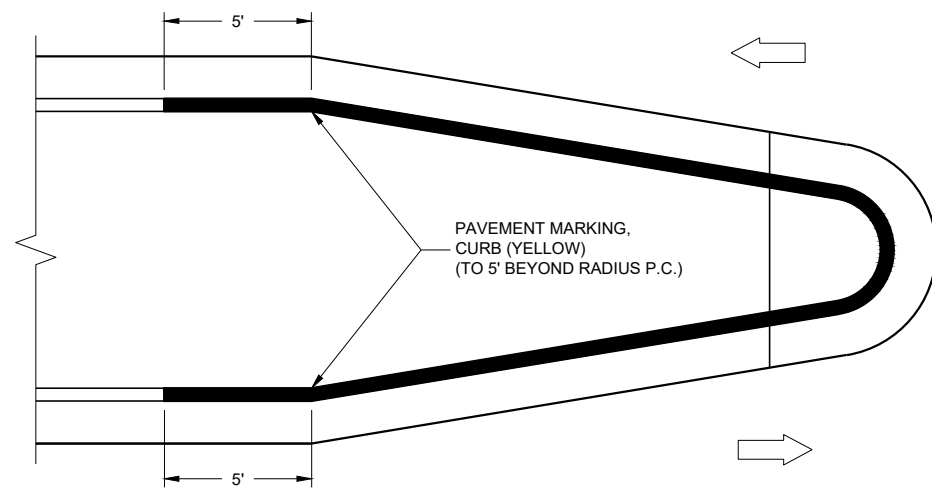
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2022 DATE /S/ Jeannie Silver
STATE SIGNING AND MARKING
ENGINEER

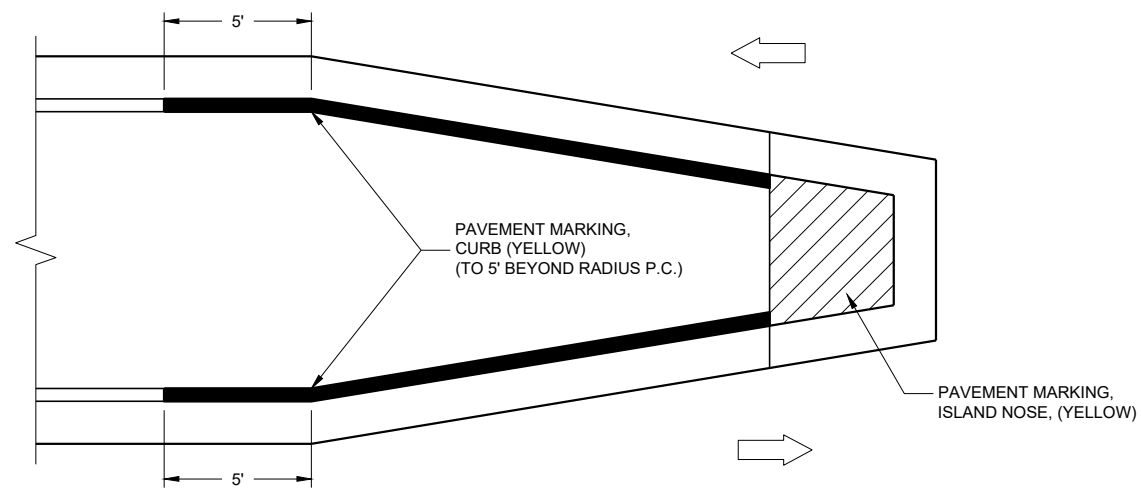
FHWA



MEDIAN ISLAND WITH SQUARE BLUNT NOSE



MEDIAN ISLAND WITH ROUND BLUNT NOSE



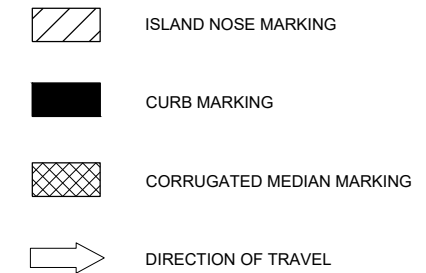
MEDIAN ISLAND WITH SLOPED NOSE

TYPICAL PLACEMENT OF PAVEMENT MARKING ON MEDIAN ISLANDS

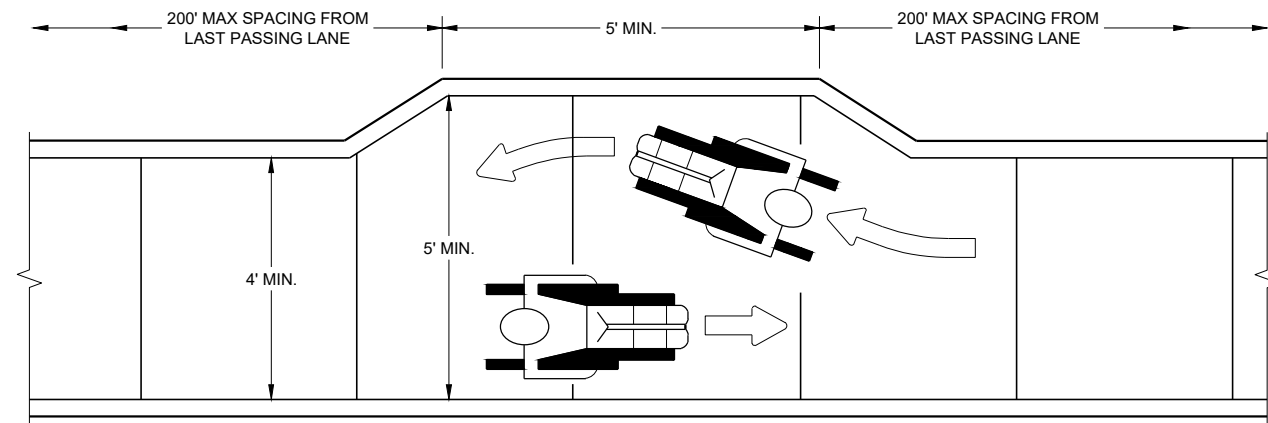
GENERAL NOTES

WHEN CONCRETE CORRUGATED MEDIAN IS CONSTRUCTED TO SEPARATE TRAFFIC OPERATING IN THE OPPOSING DIRECTION, YELLOW PAVEMENT MARKING SHALL BE APPLIED TO THE FLAT PORTION OF THE CONCRETE CORRUGATED MEDIAN. THE ITEM OF PAVEMENT MARKING, CONCRETE CORRUGATED MEDIAN, WILL BE MEASURED IN PLACE AND ACCEPTED IN ACCORDANCE WITH THE CONTRACT AND PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE FOOT.

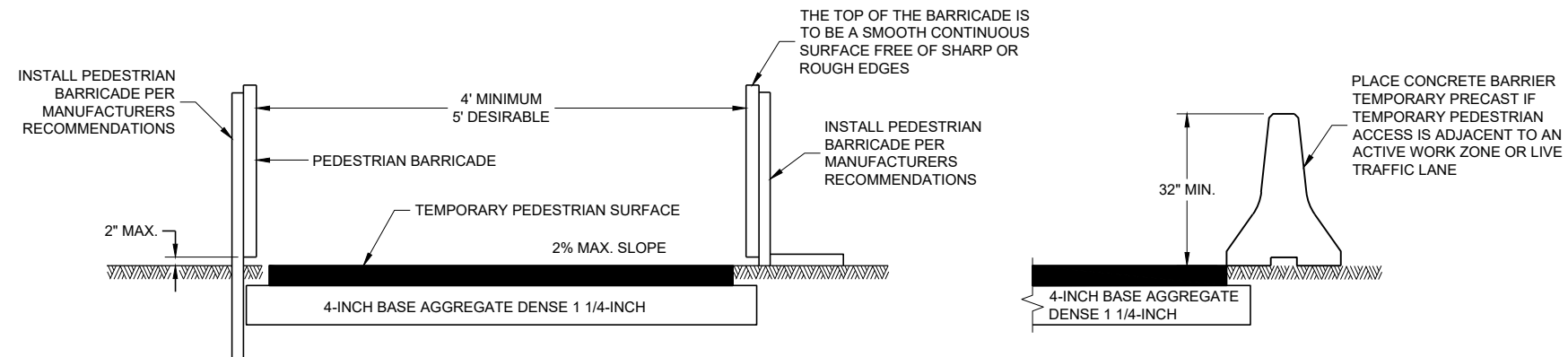
- ① APPLY PAVEMENT MARKING TO THE FLAT PORTION OF CORRUGATED MEDIAN.



| | |
|--|---|
| PAVEMENT MARKINGS, MEDIAN ISLAND NOSE | |
| STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION | |
| APPROVED May 2022 DATE | /S/ Jeannie Silver STATE SIGNING AND MARKING ENGINEER |
| <small>FHWA</small> | |



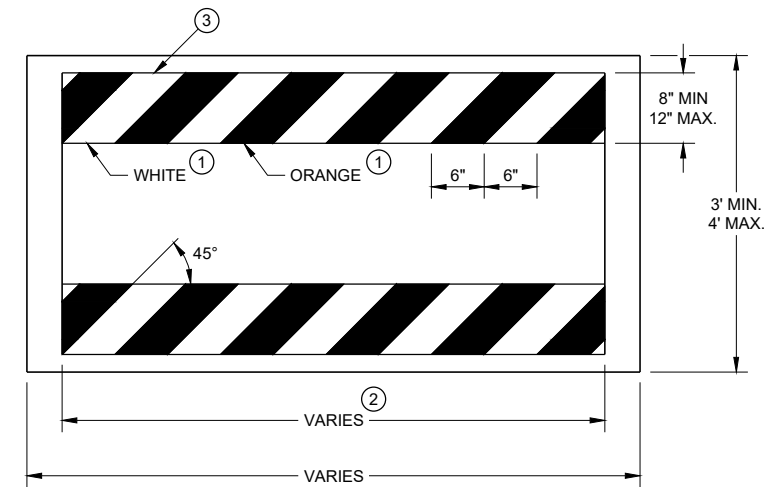
NARROW SIDEWALK PASSING DETAIL



TEMPORARY PEDESTRIAN ACCESS

GENERAL NOTES

- BARRICADE DEVICE SELECTED FROM APPROVED PRODUCT LIST
- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② SHEETING REQUIRED ON MORE THAN 50% OF BARRICADE WIDTH.
- ③ PLACE SHEETING ON BOTH SIDES OF THE BARRICADE.
- * USE THIS DETAIL FOR SHEETING PLACEMENT REFERENCE.

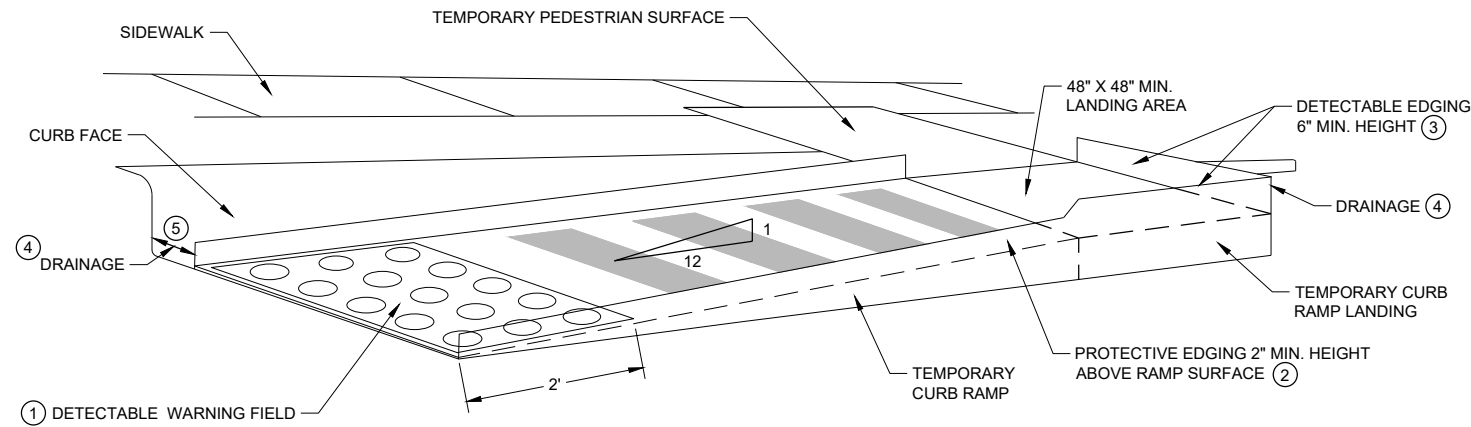


TEMPORARY PEDESTRIAN BARRICADE*

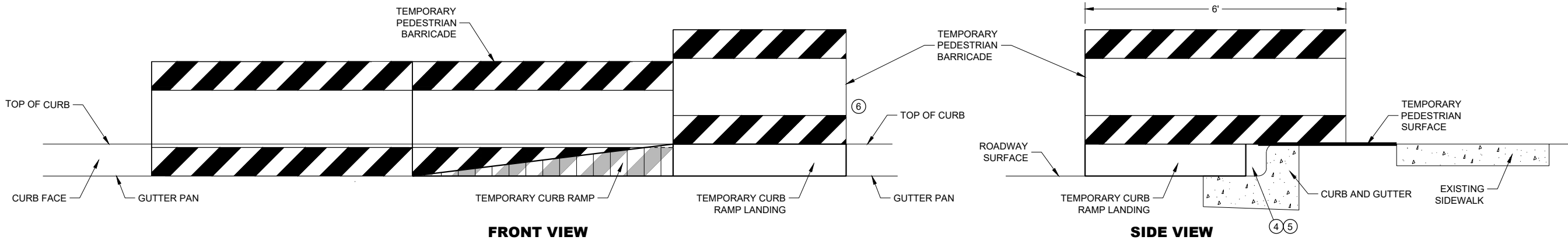
GENERAL NOTES

CURB RAMPS SHALL BE 48" MIN. WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE.
 CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.
 CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.
 LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.
 CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES MAY BE VERTICAL UP TO 1/4" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".

- ① INSTALL CONTRASTING TEMPORARY DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS, AS SHOWN IN THE PLANS.
- ② PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- ③ DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- ④ DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ⑤ 6" MINIMUM BETWEEN CURB FACE AND EDGE OF RAMP
- ⑥ IF ONLY PART OF THE END PANEL OF TEMPORARY PEDESTRIAN BARRICADE PANEL IS NEEDED, EXTEND EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL HERE.



PERSPECTIVE VIEW

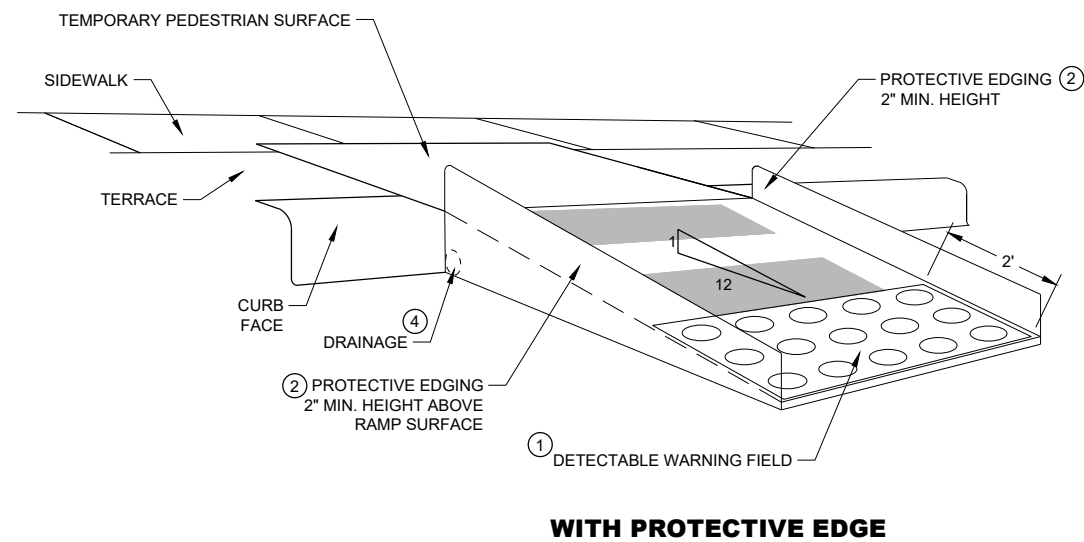
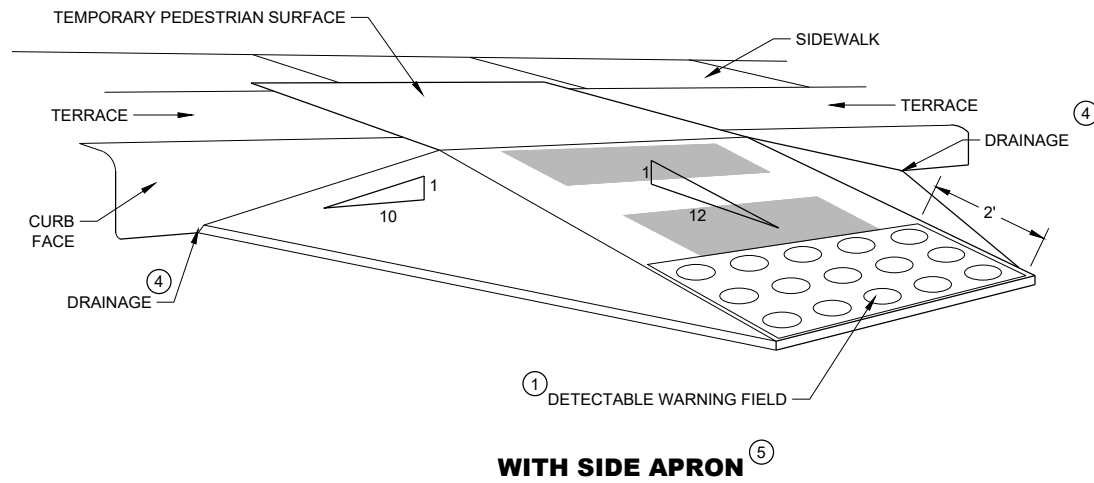


FRONT VIEW

SIDE VIEW

TEMPORARY CURB RAMP PARALLEL TO CURB

| |
|---|
| <p>TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION</p> |
| <p>STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION</p> |



TEMPORARY CURB RAMP PERPENDICULAR TO CURB

GENERAL NOTES

CURB RAMPS SHALL BE 48" MINIMUM WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE.

ALTERNATE SIDEWALK WORK BETWEEN LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.

CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.







CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.

LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.

CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES MAY BE VERTICAL UP TO 1/4" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".

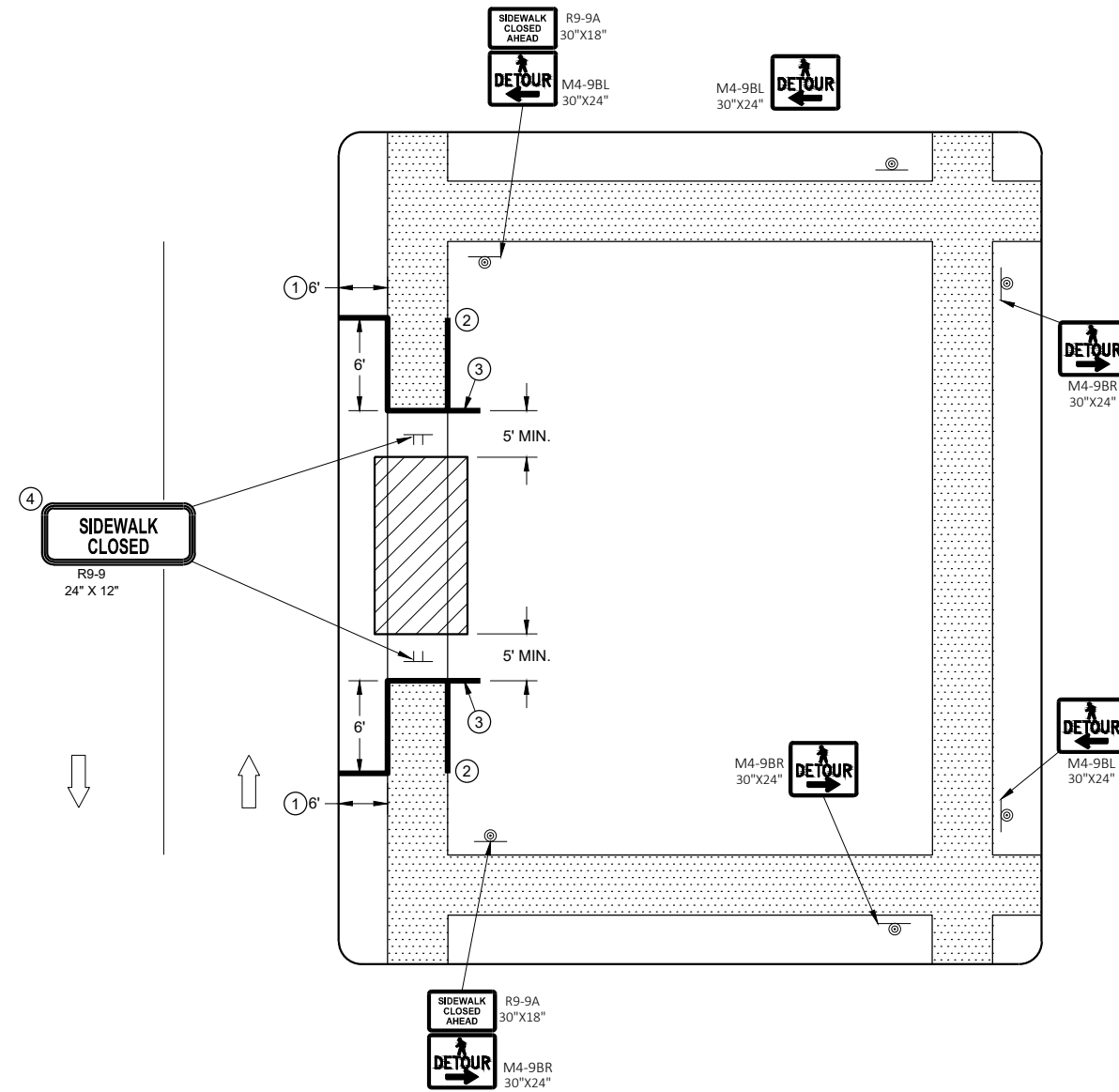
- ① INSTALL CONTRASTING TEMPORARY DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS, AS SHOWN IN THE PLANS
- ② PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- ③ DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- ④ DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ⑤ CAN ONLY BE USED FOR RAMPS WITH 6" OR LESS OF VERTICAL CHANGE.

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  SIGN ON TEMPORARY SUPPORT
-  UNDER PEDESTRIAN TRAFFIC
-  WORK AREA
-  TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC

GENERAL NOTES

- SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.
- WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.
- SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.
- ① IF TERRACE IS LESS THAN 6 FEET WIDE, OMIT TEMPORARY PEDESTRIAN BARRICADE FROM THE SIDEWALK TO THE CURB.
 - ② PLACE BARRICADE CLOSURE SO THAT THE TEMPORARY PEDESTRIAN BARRICADE END IS AT THE LAST OPEN SIDEWALK ACCESS TO RESIDENCES OR BUSINESSES BEFORE THE SIDEWALK CLOSURE.
 - ③ IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
 - ④ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.

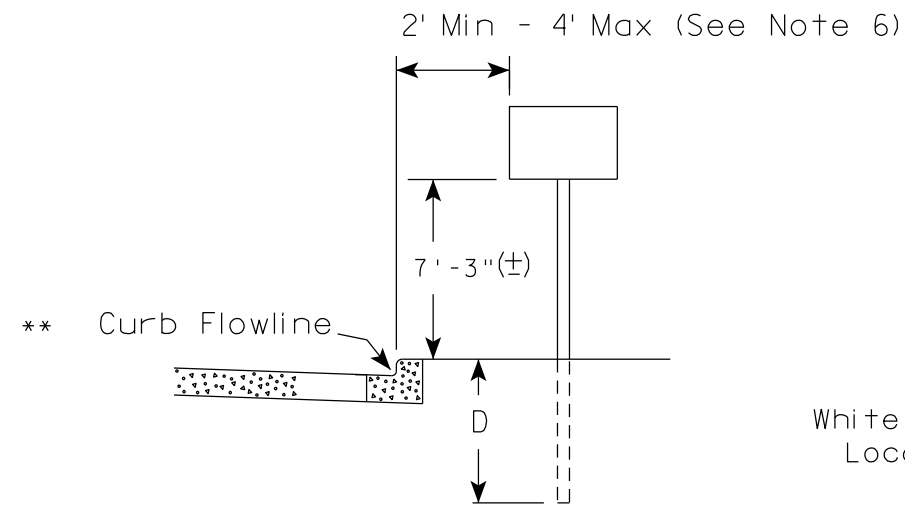


SIDEWALK DETOUR, SIDEWALK ONLY ON ONE SIDE

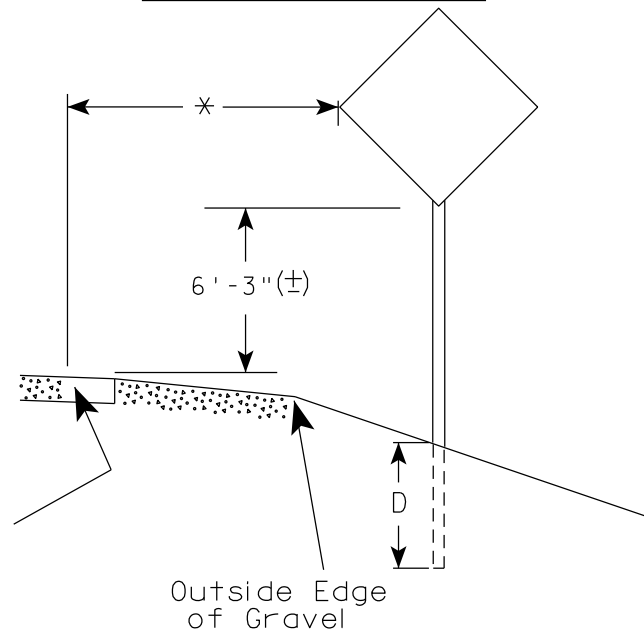
| |
|--|
| TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION |
| STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION |

URBAN AREA

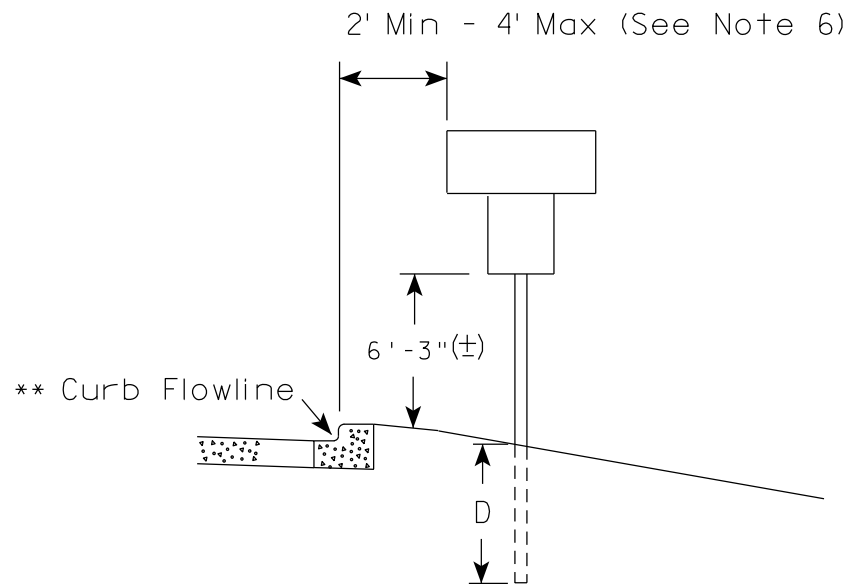
RURAL AREA (See Note 2)



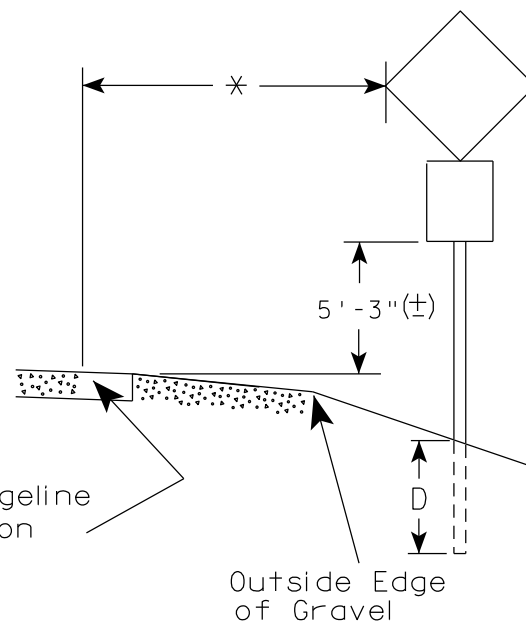
White Edgeline Location



Outside Edge of Gravel



White Edgeline Location



Outside Edge of Gravel

POST EMBEDMENT DEPTH

| Area of Sign Installation (Sq. Ft.) | D (Min) |
|---------------------------------------|-----------|
| 20 or Less | 4' |
| Greater than 20 | 5' |

GENERAL NOTES

1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on or behind barrier wall, see A4-10 sign plate.
The Double Arrow sign (W12-1D) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).
3. For expressways and freeways, mounting height is 7'- 3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
4. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±).
5. Offset distance shall be consistent with existing signs or consistent throughout length of project.
6. The (±) tolerance for mounting height is 3 inches.
7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the Engineer.

* * The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

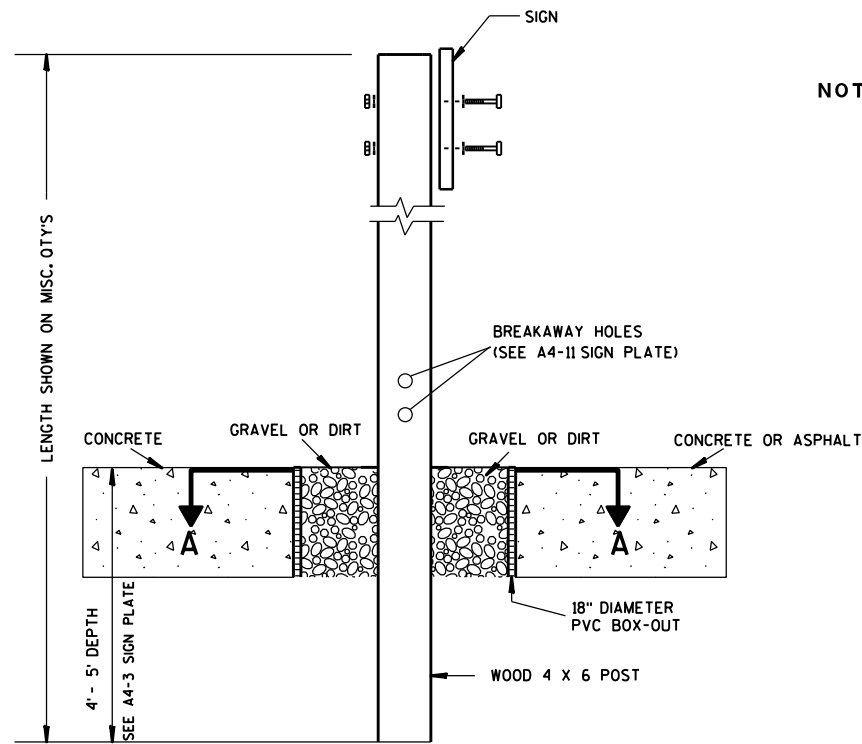
* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

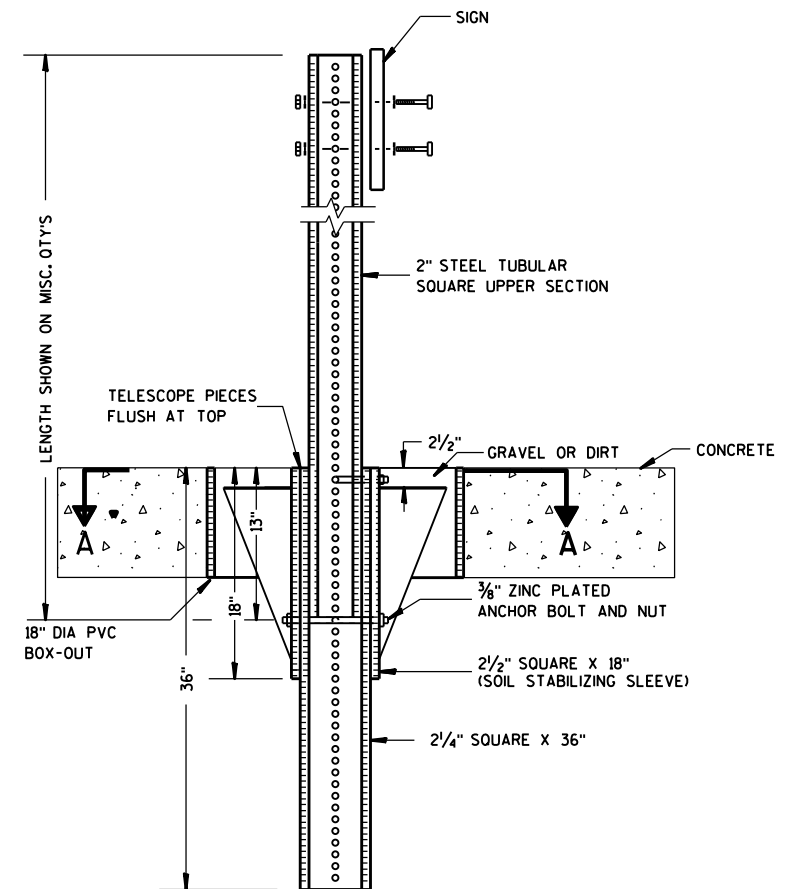
DATE 5/13/2020 PLATE NO. A4-3.22



ELEVATION VIEW

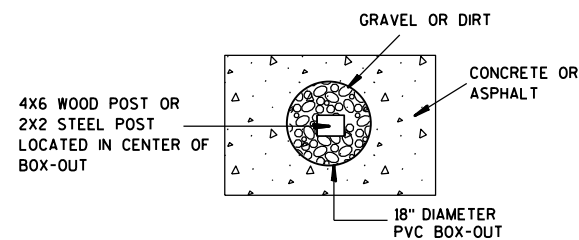
DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

- NOTES:**
1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
 2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
 3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



PLAN VIEW

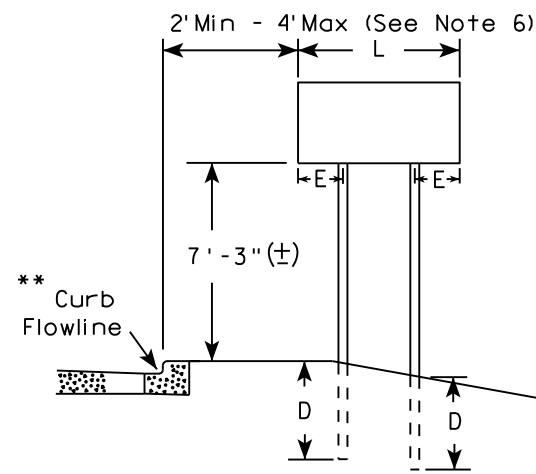
FOR NEW CONCRETE/ ASPHALT INSTALLATIONS

| | |
|---|----------------------------------|
| SIGN POST BOX-OUTS A4-3B | |
| <small>WISCONSIN DEPT OF TRANSPORTATION</small> | |
| APPROVED <i>Matthew R. Rauch</i> <small>for State Traffic Engineer</small> | |
| <small>DATE 1/27/14</small> | <small>PLATE NO. A4-3B.1</small> |

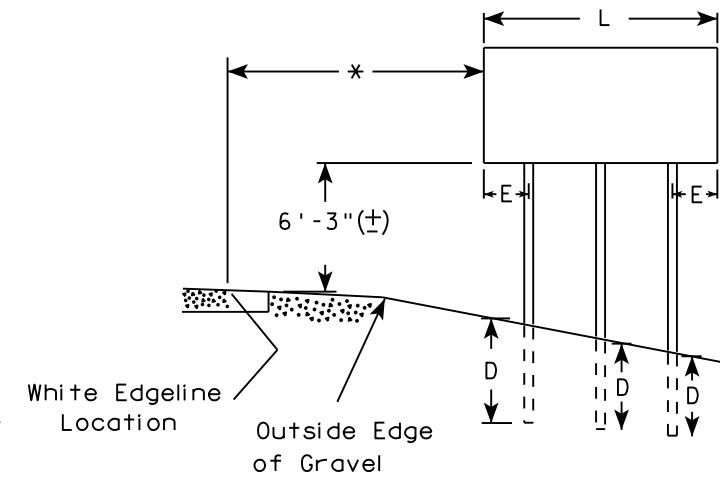
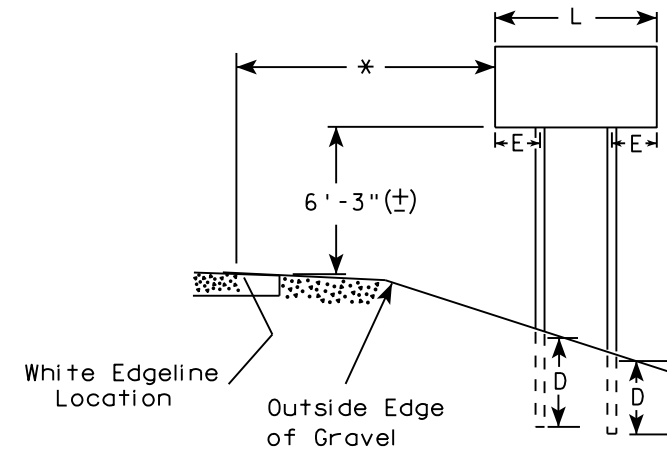
GENERAL NOTES

1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
2. See tables below for required number of posts.
3. For expressways and freeways, mounting height is 7'-3" (±) or 6'-3" (±) depending upon existence of sub-sign.
4. The (±) tolerance for mounting height is 3 inches.
5. J-Assemblies are considered to be one sign for mounting height.
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the engineer.
8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).

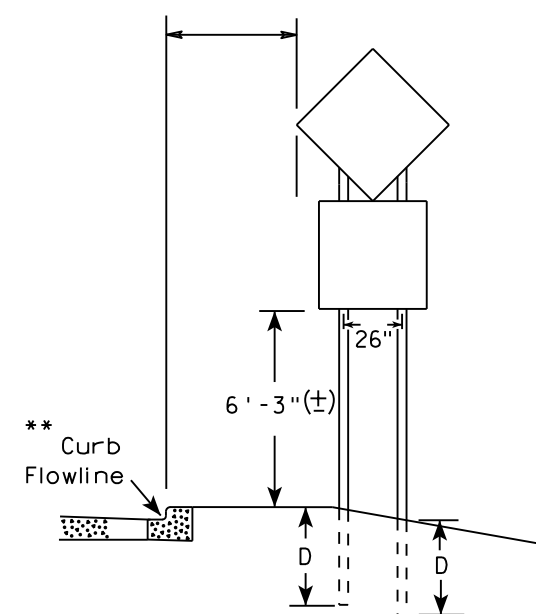
URBAN AREA



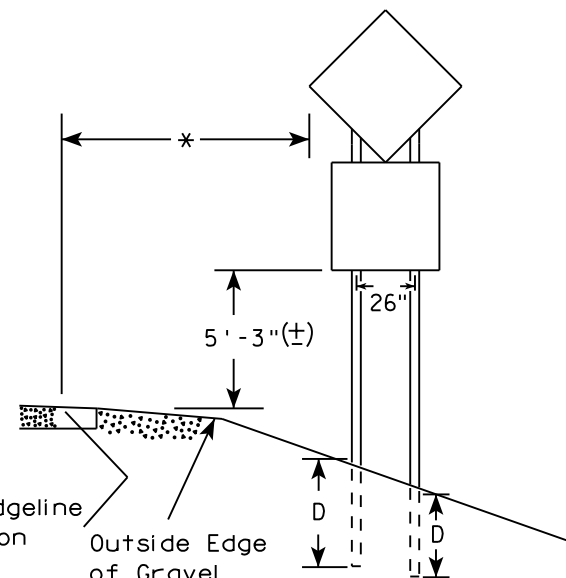
RURAL AREA (See Note 3)



2' Min - 4' Max (See Note 6)



48" DIAMOND WARNING SIGN



48" DIAMOND WARNING SIGN

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

*** See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

| SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED) | |
|--|-----|
| L | E |
| Greater than 48" Less than 60" | 12" |
| 60" to 108" | L/5 |

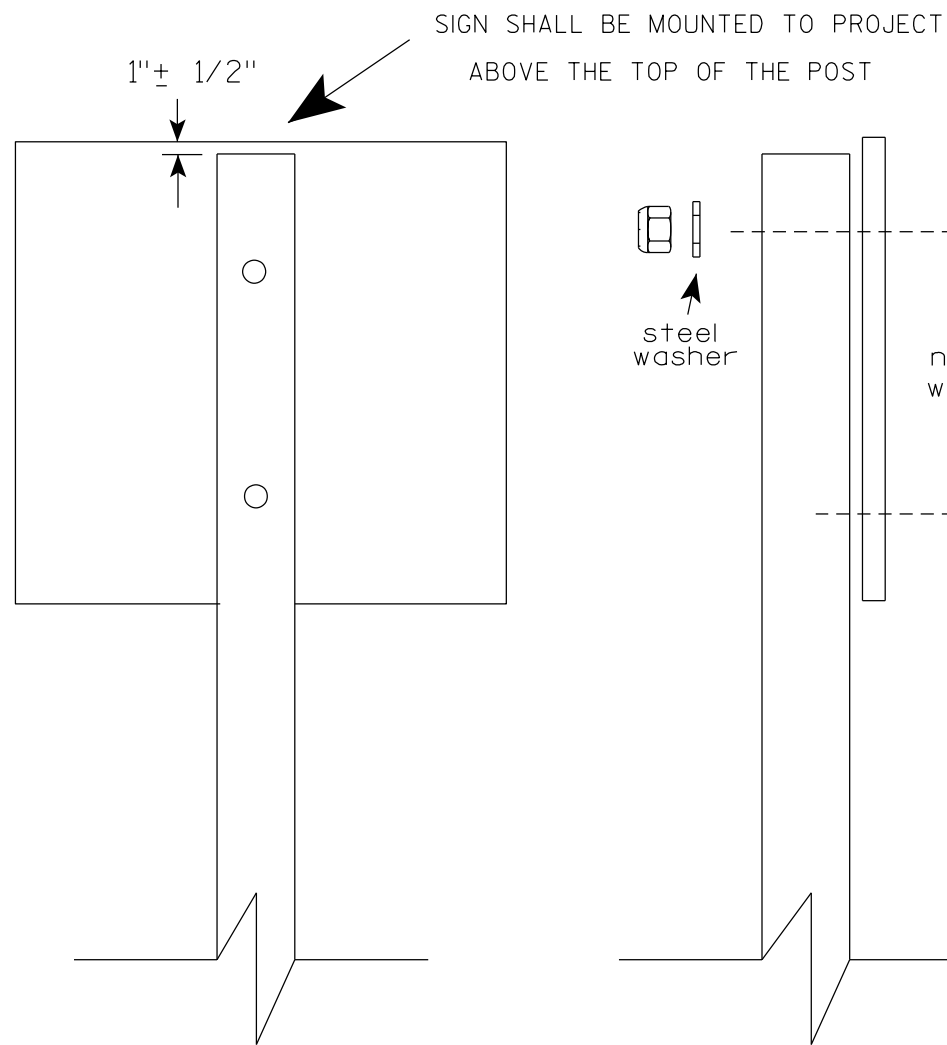
| SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED) | |
|--|-----|
| L | E |
| Greater than 108" to 144" | 12" |

POST EMBEDMENT DEPTH

| Area of Sign Installation (Sq. Ft.) | D (Min) |
|---------------------------------------|-----------|
| 20 or Less | 4' |
| Greater than 20 | 5' |

TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS

WISCONSIN DEPT OF TRANSPORTATION
 APPROVED *Matthew R. Rauch*
 For State Traffic Engineer
 DATE 8/21/17 PLATE NO. A4-4.15



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

- Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
- Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

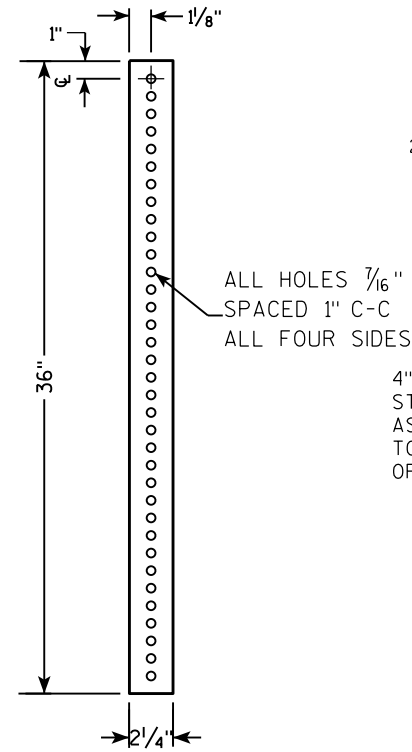
- STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)
- MACHINE BOLTS - $\frac{5}{16}$ " X 1-3/4" Length w/ lock nuts
- WOOD POSTS (4" x 6")
- LAG SCREWS - $\frac{3}{8}$ " X 3" (NO STRINGERS ON BACK OF SIGN)
 $\frac{3}{8}$ " X 4" (STRINGERS ON BACK OF SIGN)
- SQUARE STEEL POSTS (2" x 2")
- MACHINE BOLTS - $\frac{3}{8}$ " X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)
 $\frac{3}{8}$ " X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)
- RIVETS - $\frac{9}{32}$ " (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
 O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH
- WASHERS (ALL POSTS) -
- 1-1/4" O.D. X $\frac{3}{8}$ " I.D. X $\frac{1}{16}$ " STEEL
- 1-1/4" O.D. X $\frac{3}{8}$ " I.D. X .080 NYLON

* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

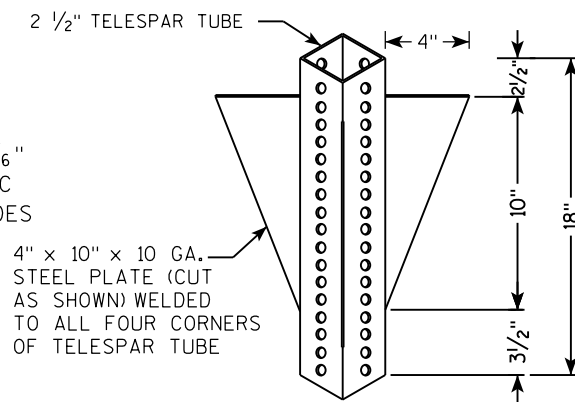
| | |
|----------------------------------|--|
| ATTACHMENT OF SIGNS TO POSTS | |
| WISCONSIN DEPT OF TRANSPORTATION | |
| APPROVED | <i>Matthew R Rauch</i> For State Traffic Engineer |
| DATE 4/1/2020 | PLATE NO. A4-8.9 |

**TELESCOPIC TUBING ANCHORS
TWO PIECE SYSTEM**

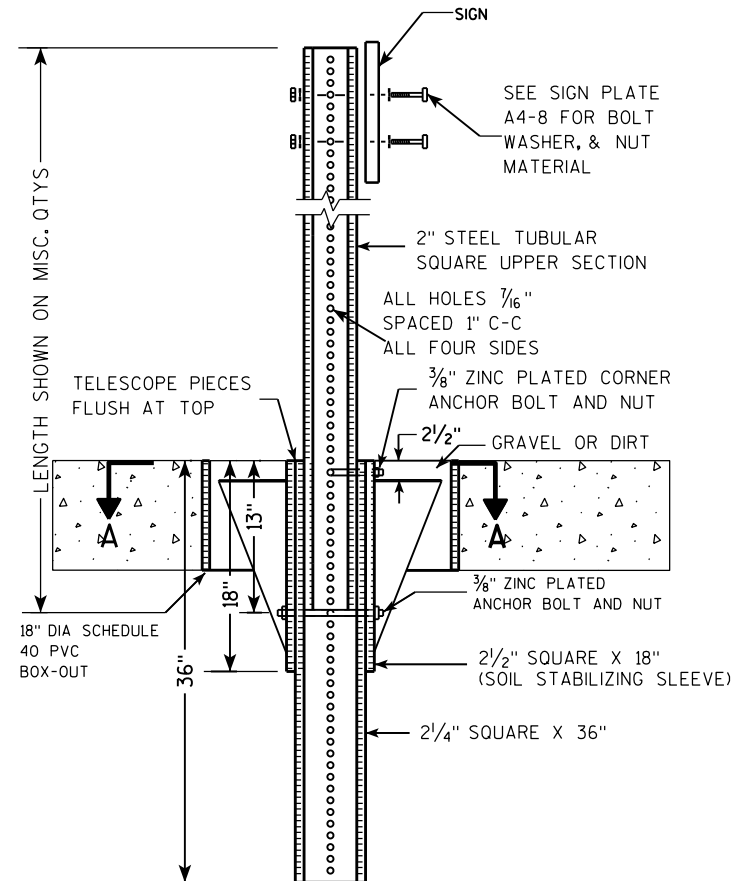
2 1/4" SQUARE
12 GAUGE
PERFORATED
GALVANIZED FINISH



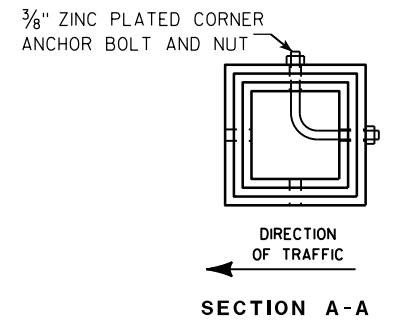
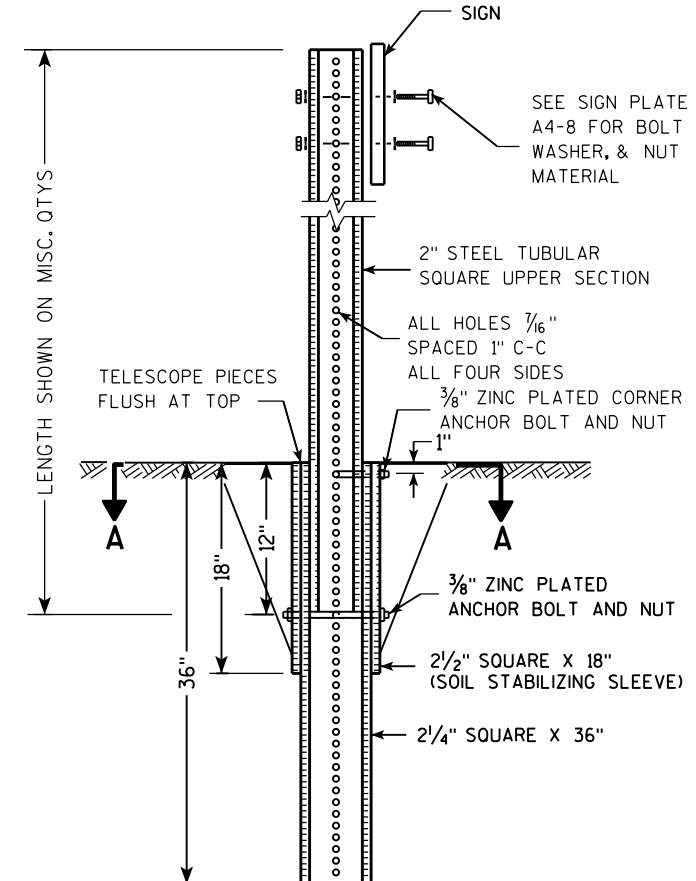
2 1/2" SQUARE
12 GAUGE
OMNI-DIRECTIONAL
PERFORATED
SOIL STABILIZING SLEEVE
GALVANIZED FINISH



**DETAIL OF TUBULAR STEEL SIGN POST
(IN POURED CONCRETE OR ASPHALT)**



**DETAIL OF TUBULAR STEEL SIGN POST
(IN LOCATIONS OTHER THAN POURED CONCRETE OR ASPHALT)**

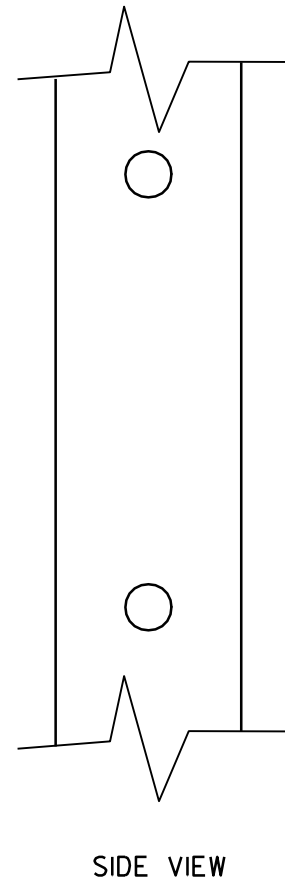
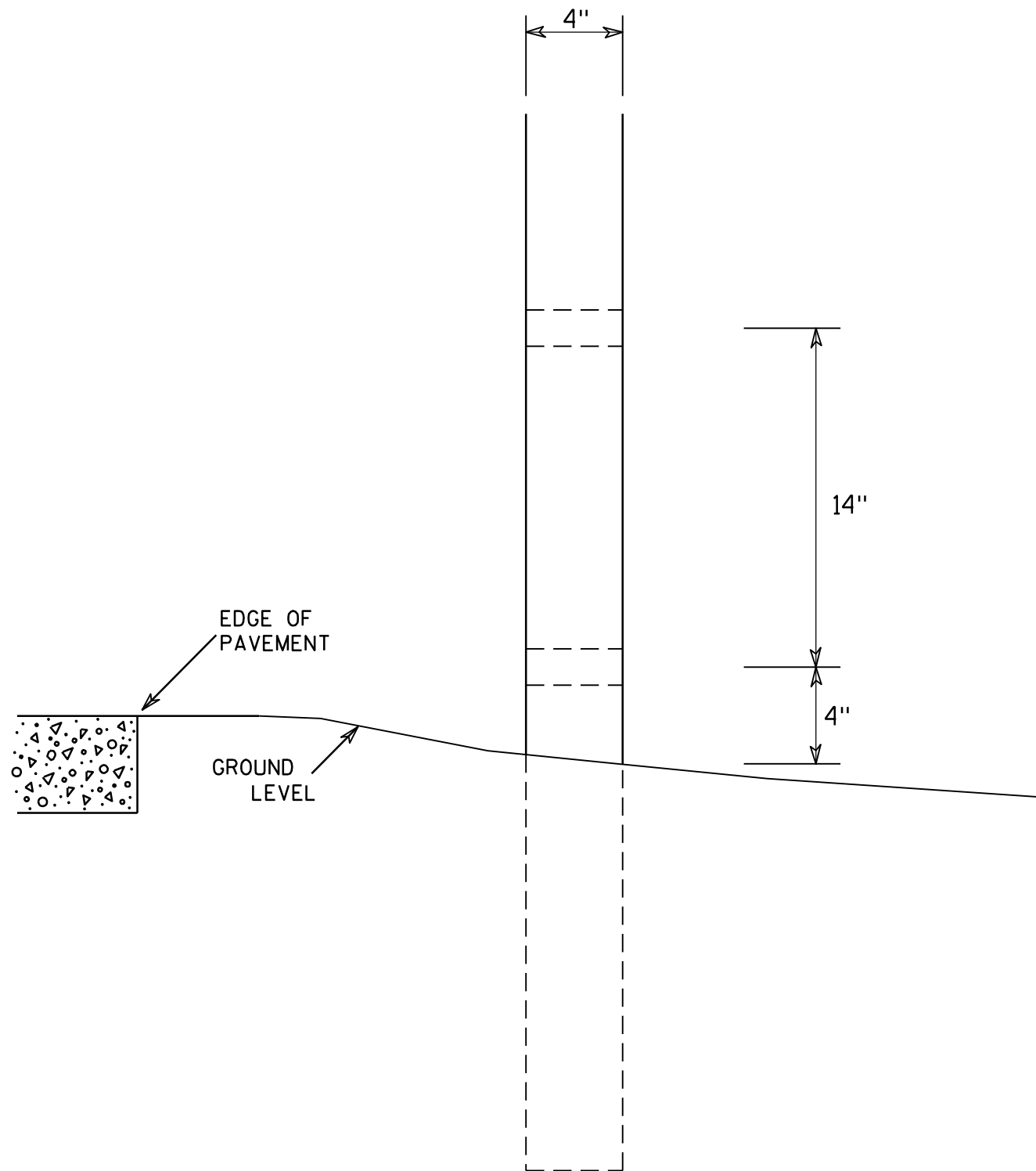


| Area of Sign Installation (Sq. Ft.) | Number of Required Posts |
|--|--------------------------|
| 9 or less | 1 |
| Greater than 9 less than or equal to 18 | 2 |
| Greater than 18 less than or equal to 27 | 3 |

Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

**TUBULAR STEEL
SIGN POST
A4-9**

WISCONSIN DEPT OF TRANSPORTATION
APPROVED *Matthew R Rauch*
for State Traffic Engineer
DATE 2/05/15 PLATE NO. A4-9.9



GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

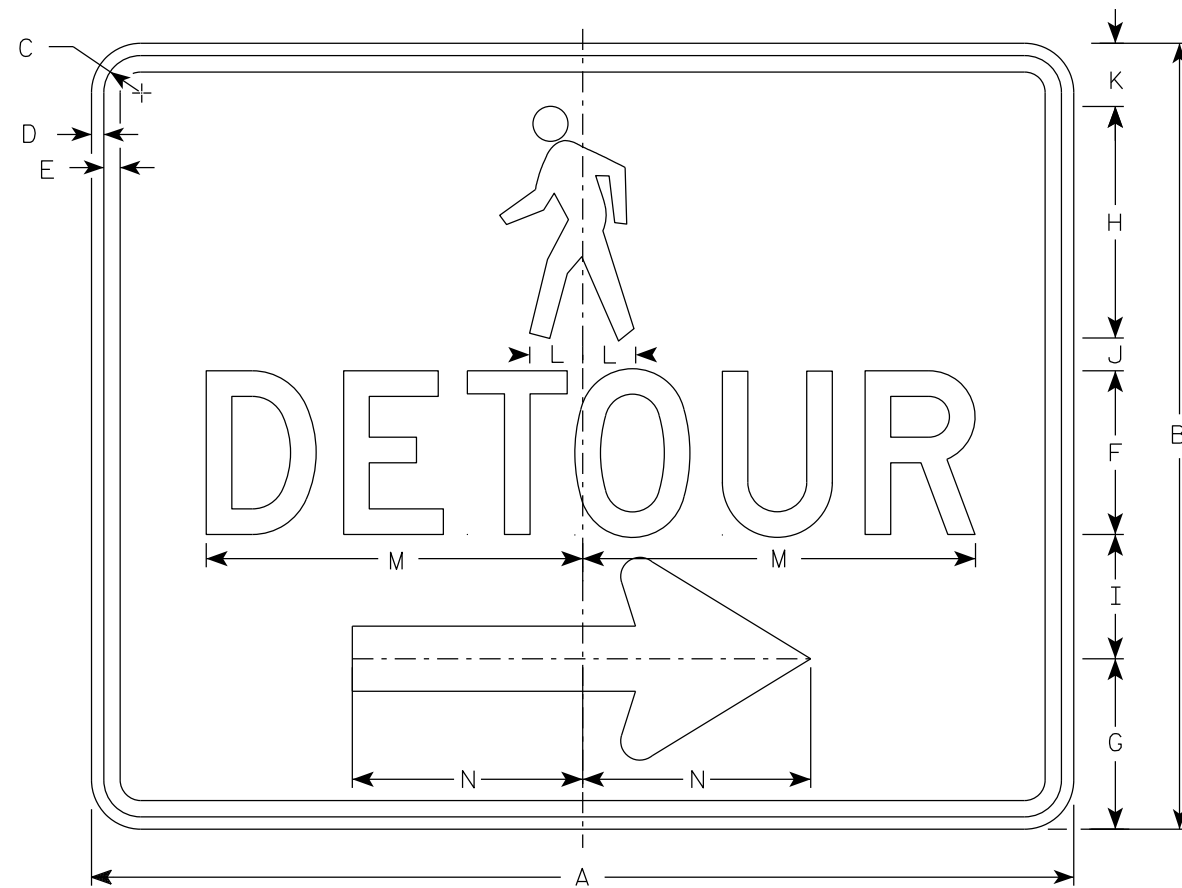
7

7

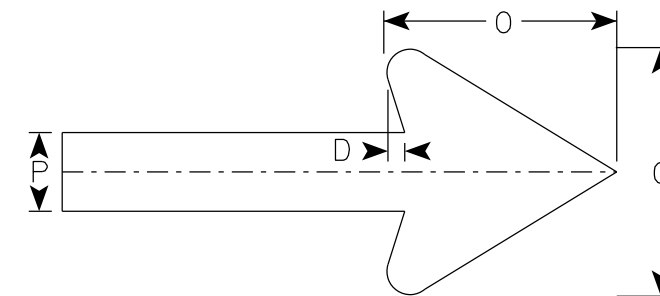
| | |
|---|--|
| 4 X 6 WOOD POST MODIFICATIONS | |
| <i>WISCONSIN DEPT OF TRANSPORTATION</i> | |
| APPROVED | <i>Chester J Spang</i> for State Traffic Engineer |
| DATE <u>3/27/97</u> | PLATE NO. <u>A4-11.2</u> |

NOTES

1. Sign is Type II-Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. M4-9BL is the same as M4-9BR except the arrow is reversed.



M4 - 9BR



Arrow Detail

| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|----|-------|-----|-----|---|-------|-------|-------|---|-------|-------|--------|---|---|---|---|---|---|---|---|---|---|---|---|---|-----------------|
| 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2 | 30 | 24 | 1 1/8 | 3/8 | 1/2 | 5 | 5 1/4 | 7 1/8 | 3 3/4 | 1 | 1 1/8 | 1 5/8 | 11 3/4 | 7 | 6 | 2 | | | | | | | | | | | 5.00 |
| 3 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 4 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 5 | | | | | | | | | | | | | | | | | | | | | | | | | | | |

STANDARD SIGN
M4-9B L&R

WISCONSIN DEPT OF TRANSPORTATION

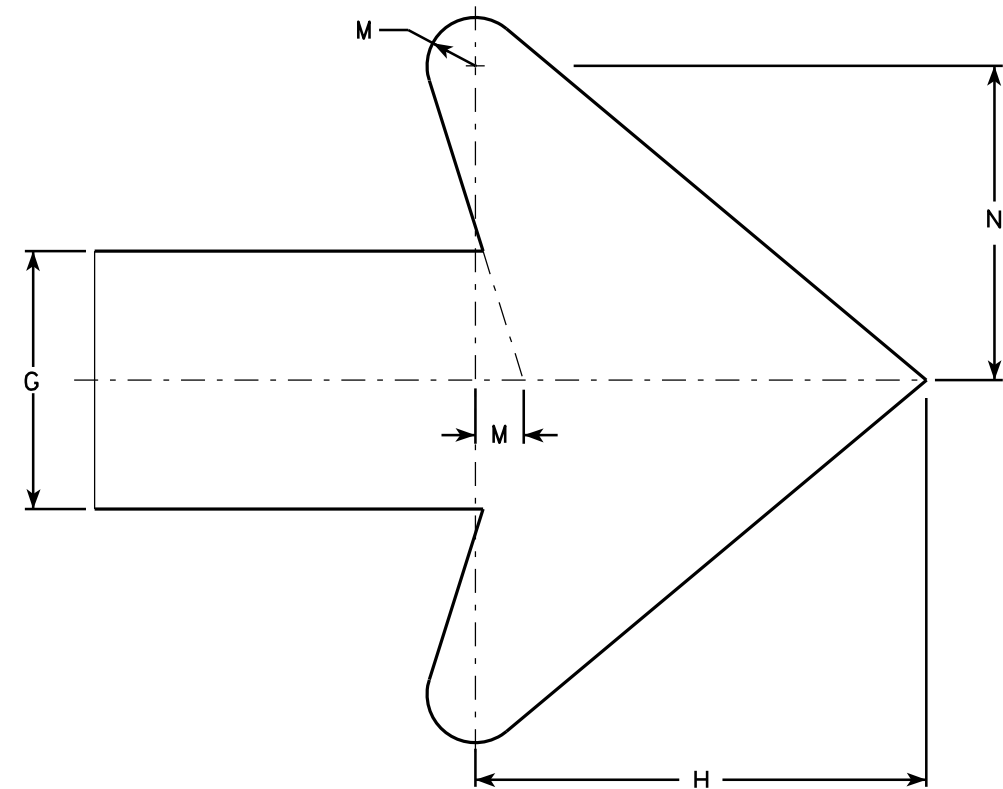
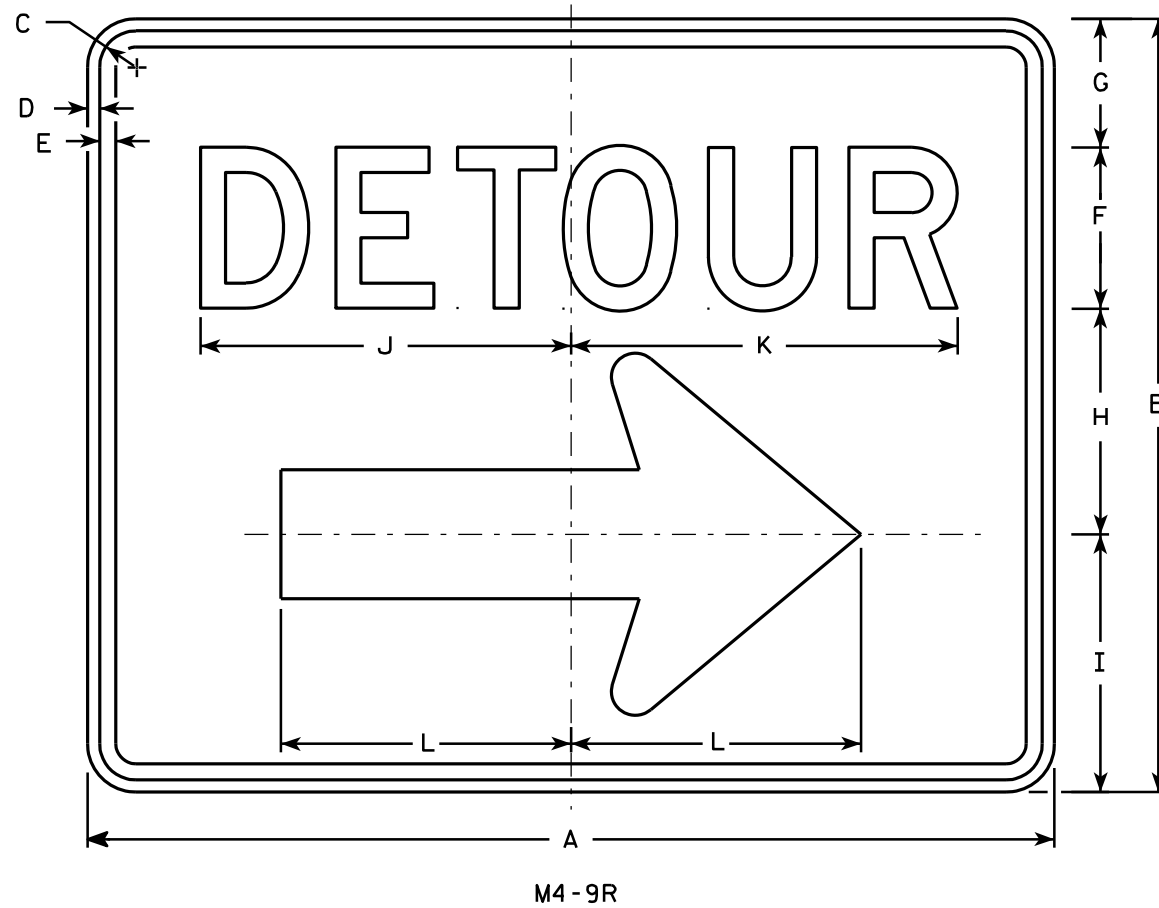
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 7/1/19 PLATE NO. M4-9B.2

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. M4-9L is the same as M4-9R except the arrow is reversed.



| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|----|-------|-----|-----|---|---|--------|--------|--------|--------|--------|-------|-------|---|---|---|---|---|---|---|---|---|---|---|---|--------------|
| 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2 | 30 | 24 | 1 1/8 | 3/8 | 1/2 | 5 | 4 | 7 | 8 | 11 1/2 | 12 | 9 | 3/4 | 4 7/8 | | | | | | | | | | | | | 5.00 |
| 3 | 30 | 24 | 1 1/8 | 3/8 | 1/2 | 5 | 4 | 7 | 8 | 11 1/2 | 12 | 9 | 3/4 | 4 7/8 | | | | | | | | | | | | | 5.00 |
| 4 | 48 | 36 | 1 3/8 | 1/2 | 5/8 | 8 | 6 | 10 1/2 | 11 5/8 | 20 5/8 | 20 1/2 | 13 1/4 | 1 1/8 | 6 7/8 | | | | | | | | | | | | | 12.0 |
| 5 | 48 | 36 | 1 3/8 | 1/2 | 5/8 | 8 | 6 | 10 1/2 | 11 5/8 | 20 5/8 | 20 1/2 | 13 1/4 | 1 1/8 | 6 7/8 | | | | | | | | | | | | | 12.0 |

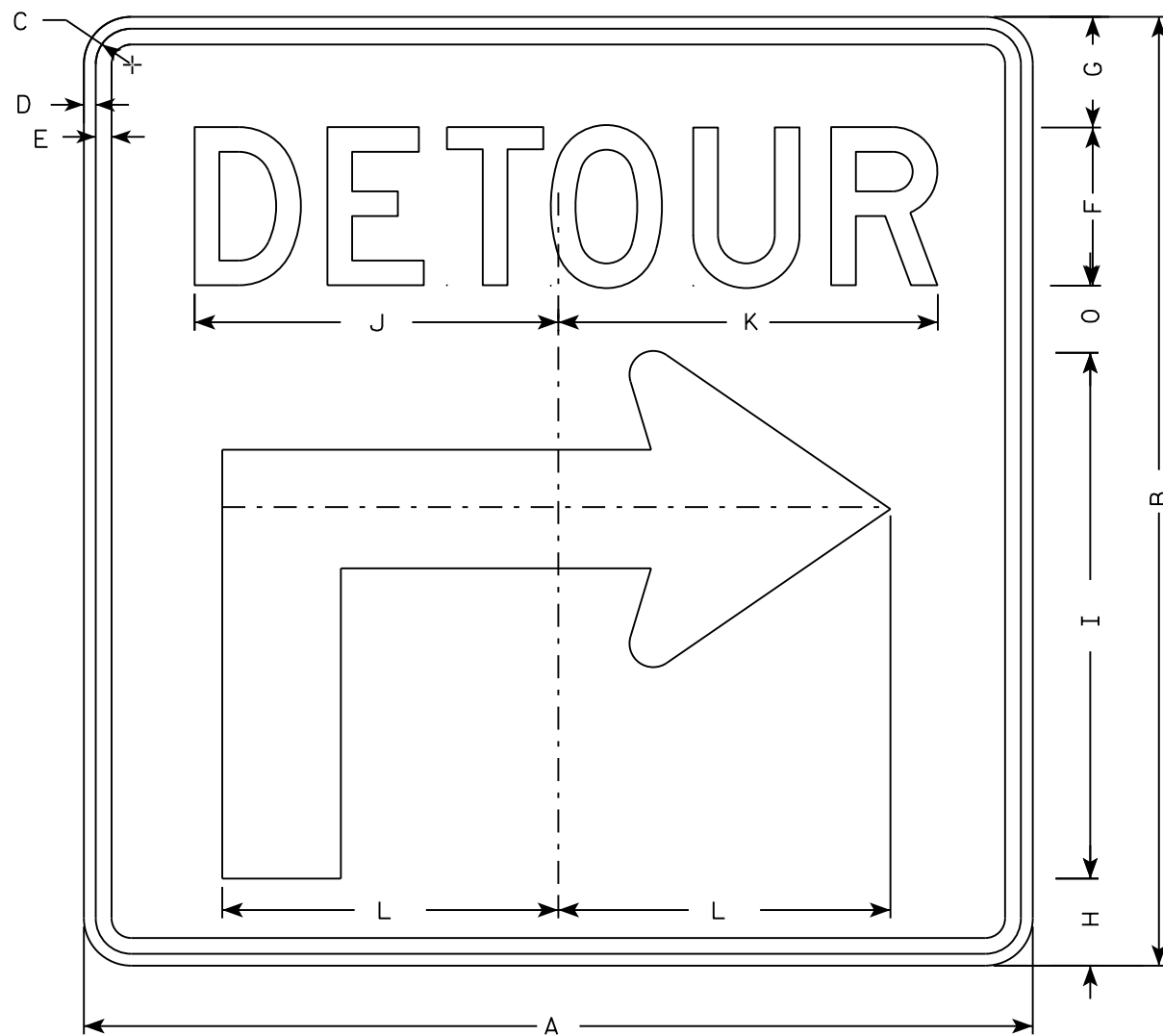
STANDARD SIGN
M4-9 R & L

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 3/9/11 PLATE NO. M4-9R.4

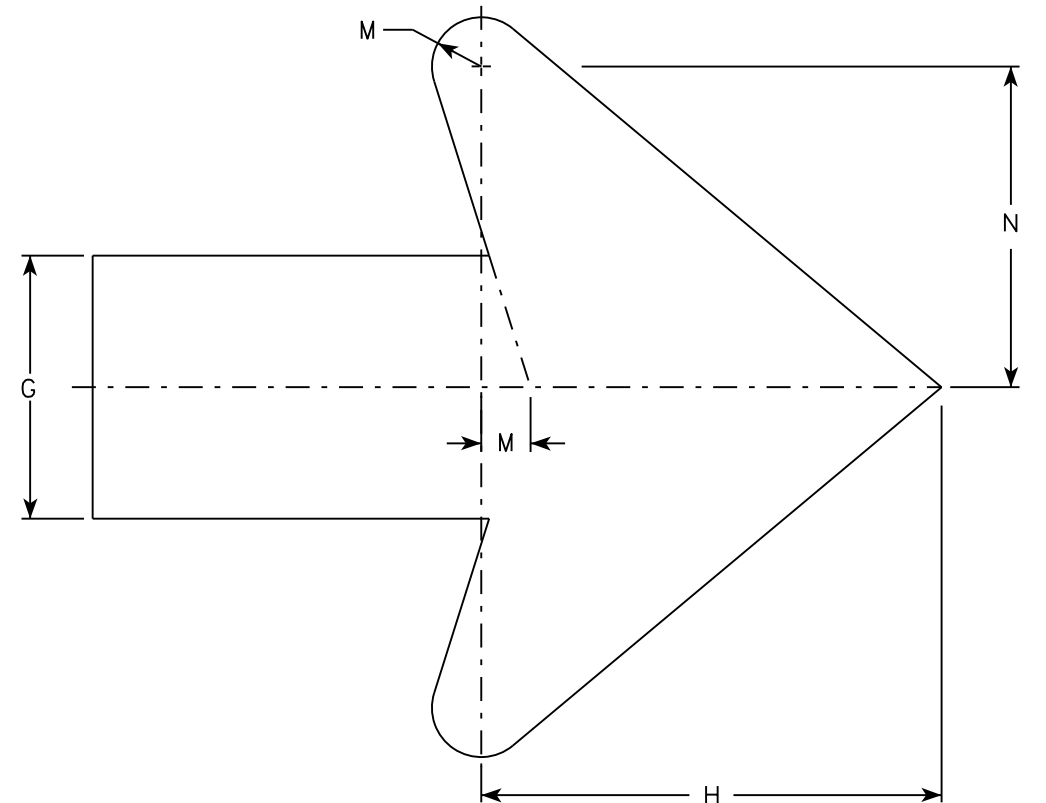
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**



M4-59R

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown when base material is metal.
5. M4-59L is the same as M4-59R except the arrow is reversed.



Arrow Detail

| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|----|-------|-----|-----|---|-------|-------|--------|--------|--------|--------|-------|-------|-------|---|---|---|---|---|---|---|---|---|---|---|--------------|
| 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2 | 30 | 30 | 1 1/8 | 3/8 | 1/2 | 5 | 3 1/2 | 2 3/4 | 16 5/8 | 11 1/2 | 12 | 10 1/2 | 3/4 | 4 7/8 | 2 1/8 | | | | | | | | | | | | 6.25 |
| 3 | 30 | 30 | 1 1/8 | 3/8 | 1/2 | 5 | 3 1/2 | 2 3/4 | 16 5/8 | 11 1/2 | 12 | 10 1/2 | 3/4 | 4 7/8 | 2 1/8 | | | | | | | | | | | | 6.25 |
| 4 | 48 | 48 | 1 3/8 | 1/2 | 5/8 | 8 | 5 5/8 | 4 3/8 | 26 5/8 | 20 5/8 | 20 1/2 | 17 | 1 1/8 | 6 7/8 | 3 3/8 | | | | | | | | | | | | 16.0 |
| 5 | 48 | 48 | 1 3/8 | 1/2 | 5/8 | 8 | 5 5/8 | 4 3/8 | 26 5/8 | 20 5/8 | 20 1/2 | 17 | 1 1/8 | 6 7/8 | 3 3/8 | | | | | | | | | | | | 16.0 |

STANDARD SIGN
M4-59 L&R

WISCONSIN DEPT OF TRANSPORTATION

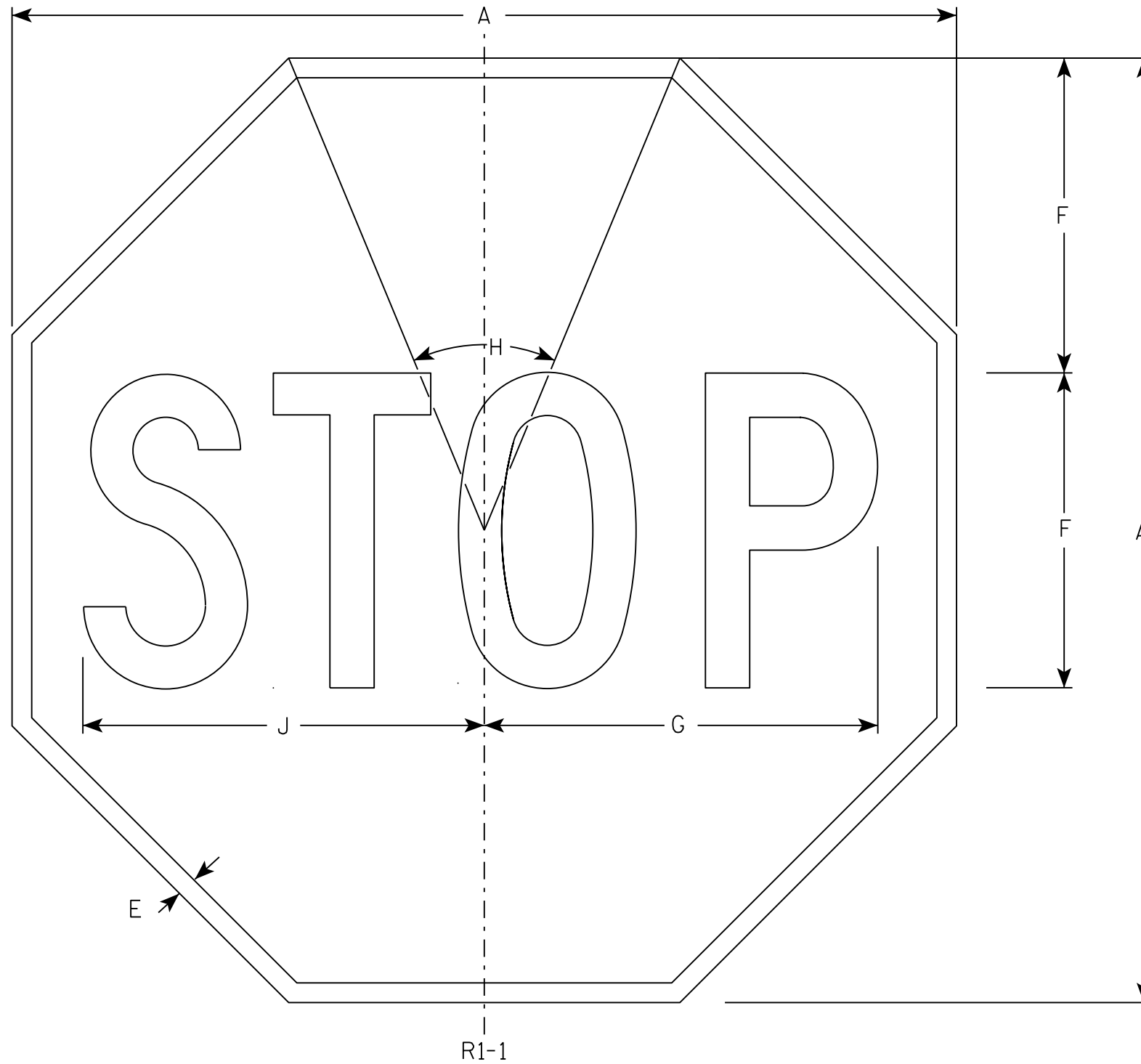
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 11/10/15 PLATE NO. M4-59.1

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ **E**

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Red
Message - White
3. Message Series - C



7

7

| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|---|---|---|-----|----|--------|-----|---|--------|---|---|---|---|---|---|---|---|---|---|---|---|---|---|---|---|--------------|
| 1 | 30 | | | | 5/8 | 10 | 12 1/2 | 45° | | 12 3/4 | | | | | | | | | | | | | | | | | 5.18 |
| 2S | 30 | | | | 5/8 | 10 | 12 1/2 | 45° | | 12 3/4 | | | | | | | | | | | | | | | | | 5.18 |
| 2M | 36 | | | | 3/4 | 12 | 15 | 45° | | 15 3/8 | | | | | | | | | | | | | | | | | 7.46 |
| 3 | 36 | | | | 3/4 | 12 | 15 | 45° | | 15 3/8 | | | | | | | | | | | | | | | | | 7.46 |
| 4 | 48 | | | | 1 | 16 | 20 | 45° | | 20 1/2 | | | | | | | | | | | | | | | | | 13.25 |
| 5 | 48 | | | | 1 | 16 | 20 | 45° | | 20 1/2 | | | | | | | | | | | | | | | | | 13.25 |
| 6 | 18 | | | | 3/8 | 6 | 7 3/4 | 45° | | 7 3/4 | | | | | | | | | | | | | | | | | 1.86 |
| 7 | 12 | | | | 1/4 | 4 | 5 | 45° | | 5 1/8 | | | | | | | | | | | | | | | | | 0.78 |

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

STANDARD SIGN
R1-1

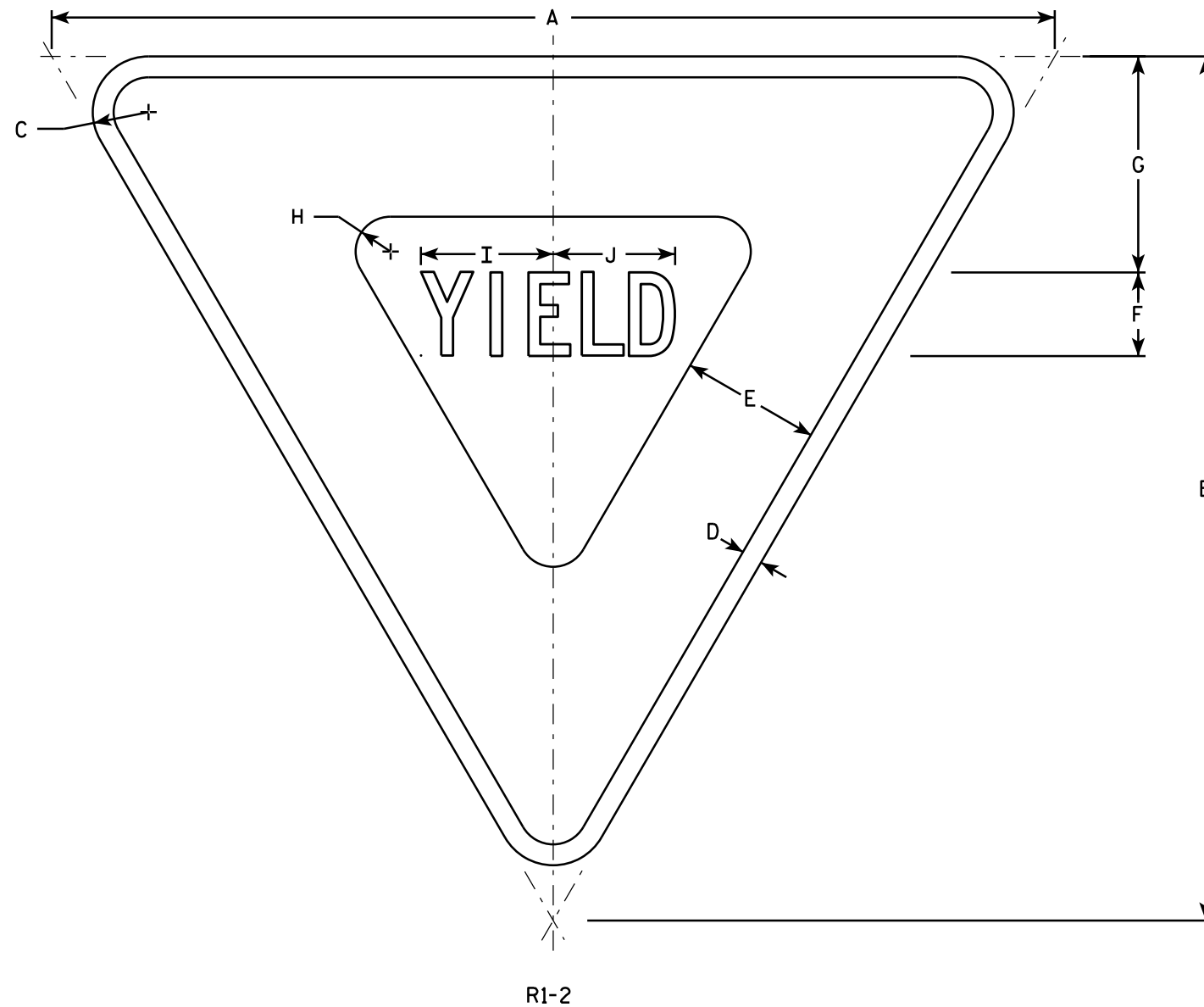
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 11/12/15 PLATE NO. R1-1.13

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - See note 5
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. The border strip and word message are reflectorized red.



7

7

| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|--------|-------|-------|-------|-------|-------|-------|-------|-------|---|---|---|---|---|---|---|---|---|---|---|---|---|---|---|---|-----------------|
| 1 | 30 | 26 | 1 1/2 | 5/8 | 4 | 2 1/2 | 6 3/8 | 7/8 | 4 | 3 5/8 | | | | | | | | | | | | | | | | | 2.71 |
| 2S | 36 | 31 | 2 | 3/4 | 5 | 3 | 7 3/4 | 1 1/4 | 4 3/4 | 4 3/8 | | | | | | | | | | | | | | | | | 3.88 |
| 2M | 48 | 42 | 3 | 1 | 6 | 4 | 9 3/4 | 2 | 6 1/4 | 5 7/8 | | | | | | | | | | | | | | | | | 7.00 |
| 3 | 48 | 42 | 3 | 1 | 6 | 4 | 9 3/4 | 2 | 6 1/4 | 5 7/8 | | | | | | | | | | | | | | | | | 7.00 |
| 4 | 48 | 42 | 3 | 1 | 6 | 4 | 9 3/4 | 2 | 6 1/4 | 5 7/8 | | | | | | | | | | | | | | | | | 7.00 |
| 5 | 60 | 52 | 3 | 1 1/2 | 8 | 5 | 13 | 2 1/2 | 7 7/8 | 7 1/4 | | | | | | | | | | | | | | | | | 10.83 |
| 6 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 7 | 18 | 15 1/2 | 1 | 3/8 | 2 1/2 | 1 1/2 | 3 7/8 | 5/8 | 2 3/8 | 2 1/4 | | | | | | | | | | | | | | | | | 0.97 |

STANDARD SIGN
R1-2

WISCONSIN DEPT OF TRANSPORTATION

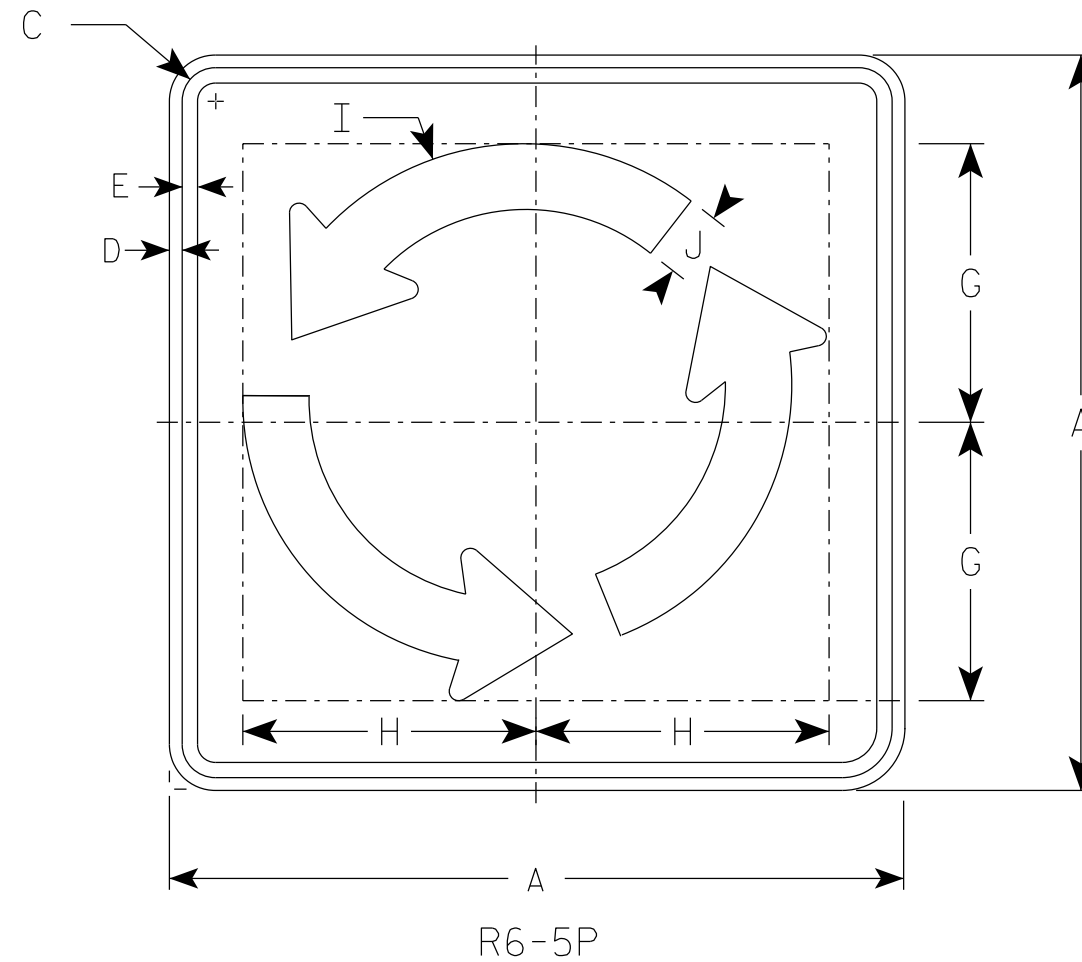
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 10/13/14 PLATE NO. R1-2.12

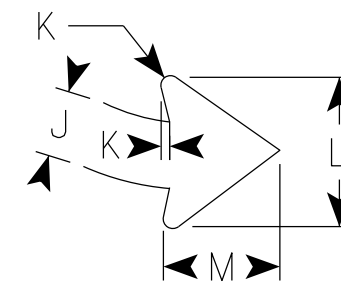
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
 Background - White
 Message - Black



Arrow Detail



7

7

| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Area sq. ft. |
|------|----|---|-------|-----|-----|---|--------|----|--------|-------|-----|---|-------|---|---|---|---|---|---|---|---|---|---|---|-----------------|
| 1 | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2S | 30 | | 1 3/8 | 1/2 | 5/8 | | 11 3/8 | 12 | 11 1/8 | 2 3/4 | 3/8 | 6 | 4 3/4 | | | | | | | | | | | | 6.25 |
| 2M | 30 | | 1 3/8 | 1/2 | 5/8 | | 11 3/8 | 12 | 11 1/8 | 2 3/4 | 3/8 | 6 | 4 3/4 | | | | | | | | | | | | 6.25 |
| 3 | | | | | | | | | | | | | | | | | | | | | | | | | |
| 4 | | | | | | | | | | | | | | | | | | | | | | | | | |
| 5 | | | | | | | | | | | | | | | | | | | | | | | | | |

STANDARD SIGN
R6-5P

WISCONSIN DEPT OF TRANSPORTATION

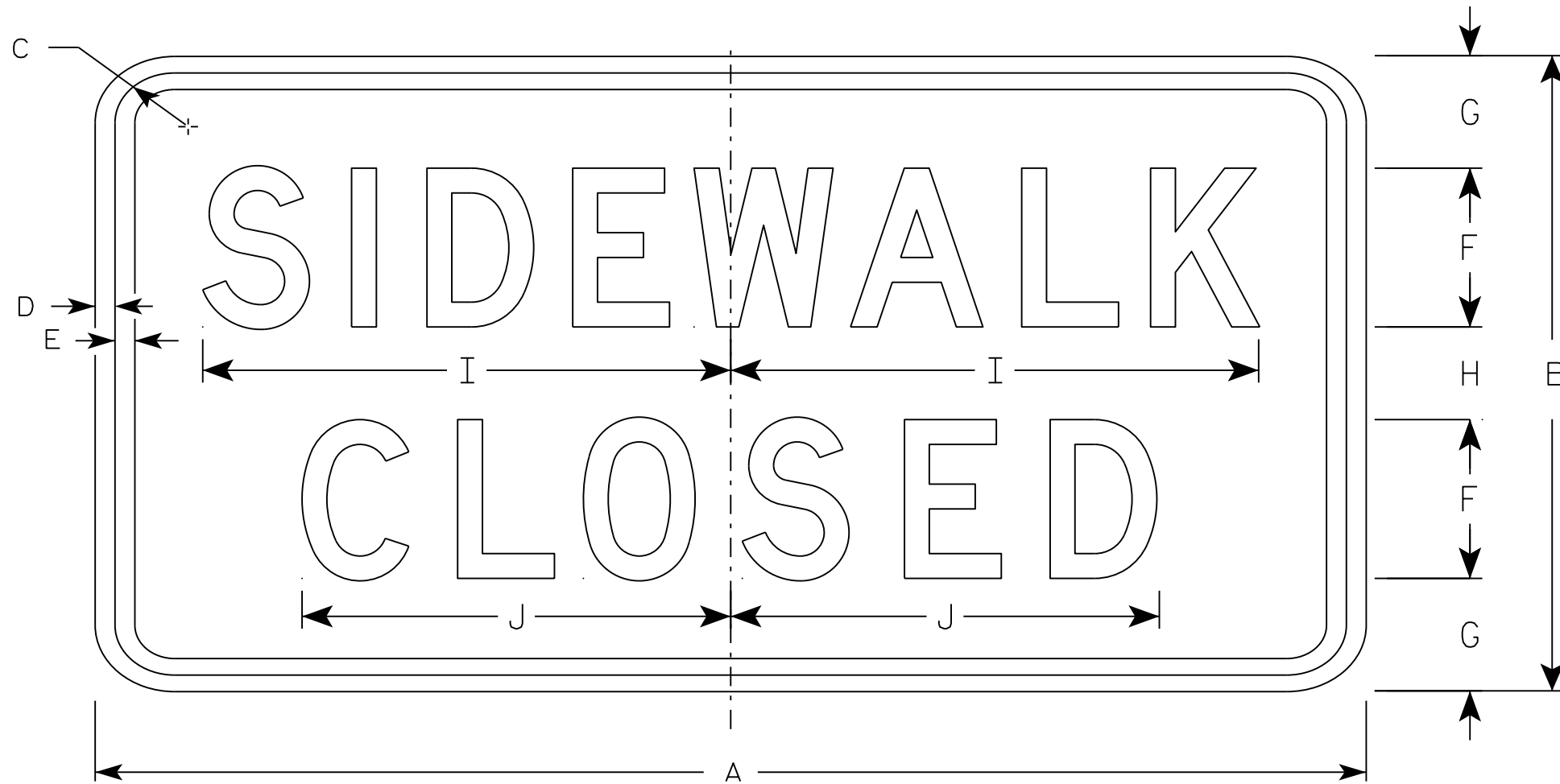
APPROVED *Matthew R Raub*
for State Traffic Engineer

DATE 3/24/21 PLATE NO. R6-5P.1

PROJECT NO: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Use Size 2 for Sidewalks. Use Size 3 for Paths and Trails.



R9-9

7

| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|----|-------|-----|-----|---|-------|-------|--------|--------|---|---|---|---|---|---|---|---|---|---|---|---|---|---|---|---|-----------------|
| 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2S | 24 | 12 | 1 3/4 | 1/2 | 1/2 | 3 | 2 1/8 | 1 3/4 | 10 | 8 1/8 | | | | | | | | | | | | | | | | | 2.0 |
| 2M | 24 | 12 | 1 3/4 | 1/2 | 1/2 | 3 | 2 1/8 | 1 3/4 | 10 | 8 1/8 | | | | | | | | | | | | | | | | | 2.0 |
| 3 | 30 | 18 | 1 3/4 | 1/2 | 1/2 | 4 | 3 1/2 | 3 | 12 1/2 | 10 1/4 | | | | | | | | | | | | | | | | | 3.75 |
| 4 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 5 | | | | | | | | | | | | | | | | | | | | | | | | | | | |

STANDARD SIGN
R9-9

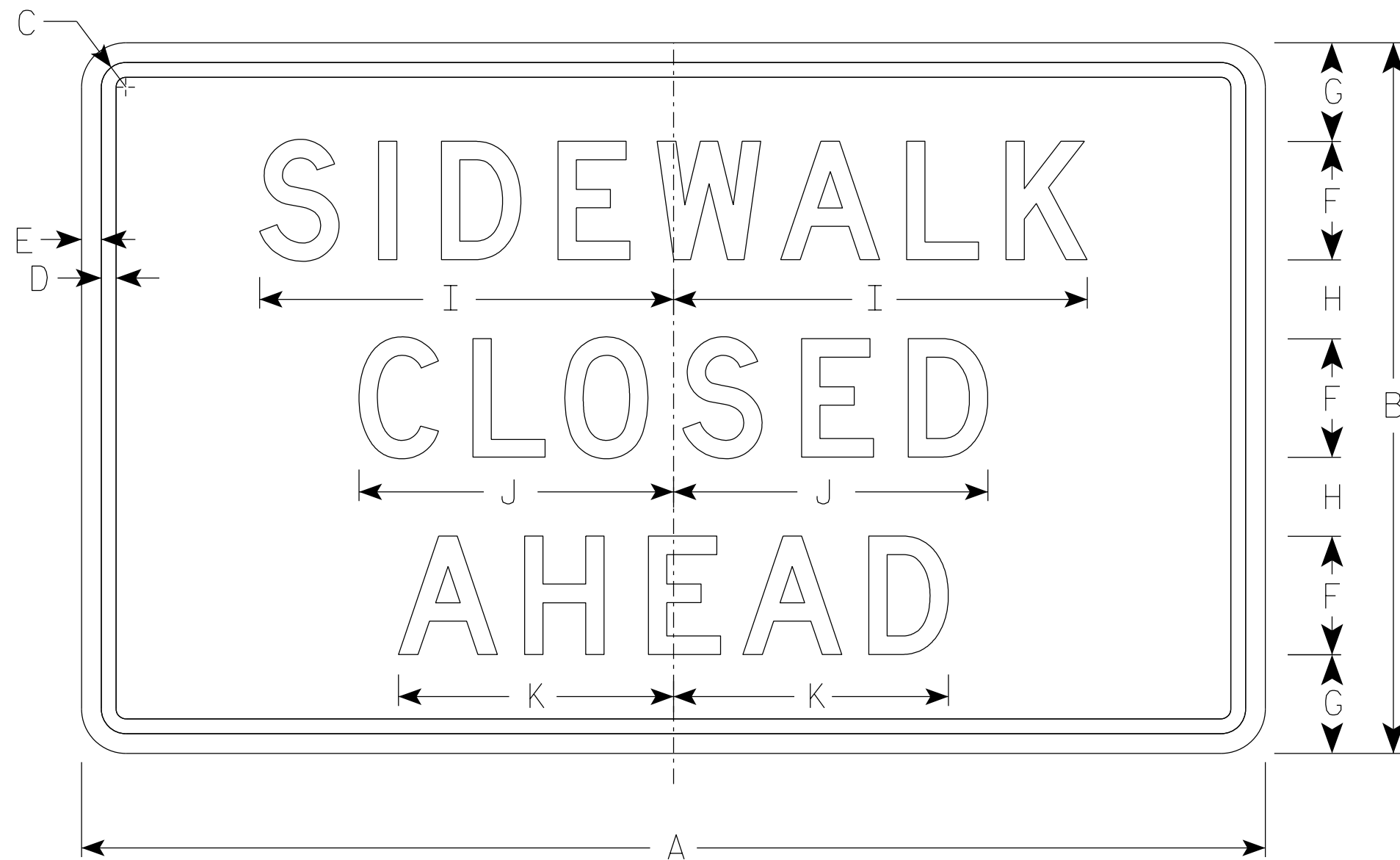
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 8/11/16 PLATE NO. R9-9.6

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



R9-9A

| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|----|-------|-----|-----|---|-------|---|--------|---|---|---|---|---|---|---|---|---|---|---|---|---|---|---|---|---|-----------------|
| 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2S | 30 | 18 | 1 1/8 | 3/8 | 1/2 | 3 | 2 1/2 | 2 | 10 1/2 | 8 | 7 | | | | | | | | | | | | | | | | 3.75 |
| 2M | 30 | 18 | 1 1/8 | 3/8 | 1/2 | 3 | 2 1/2 | 2 | 10 1/2 | 8 | 7 | | | | | | | | | | | | | | | | 3.75 |
| 3 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 4 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 5 | | | | | | | | | | | | | | | | | | | | | | | | | | | |

STANDARD SIGN
R9-9A

WISCONSIN DEPT OF TRANSPORTATION

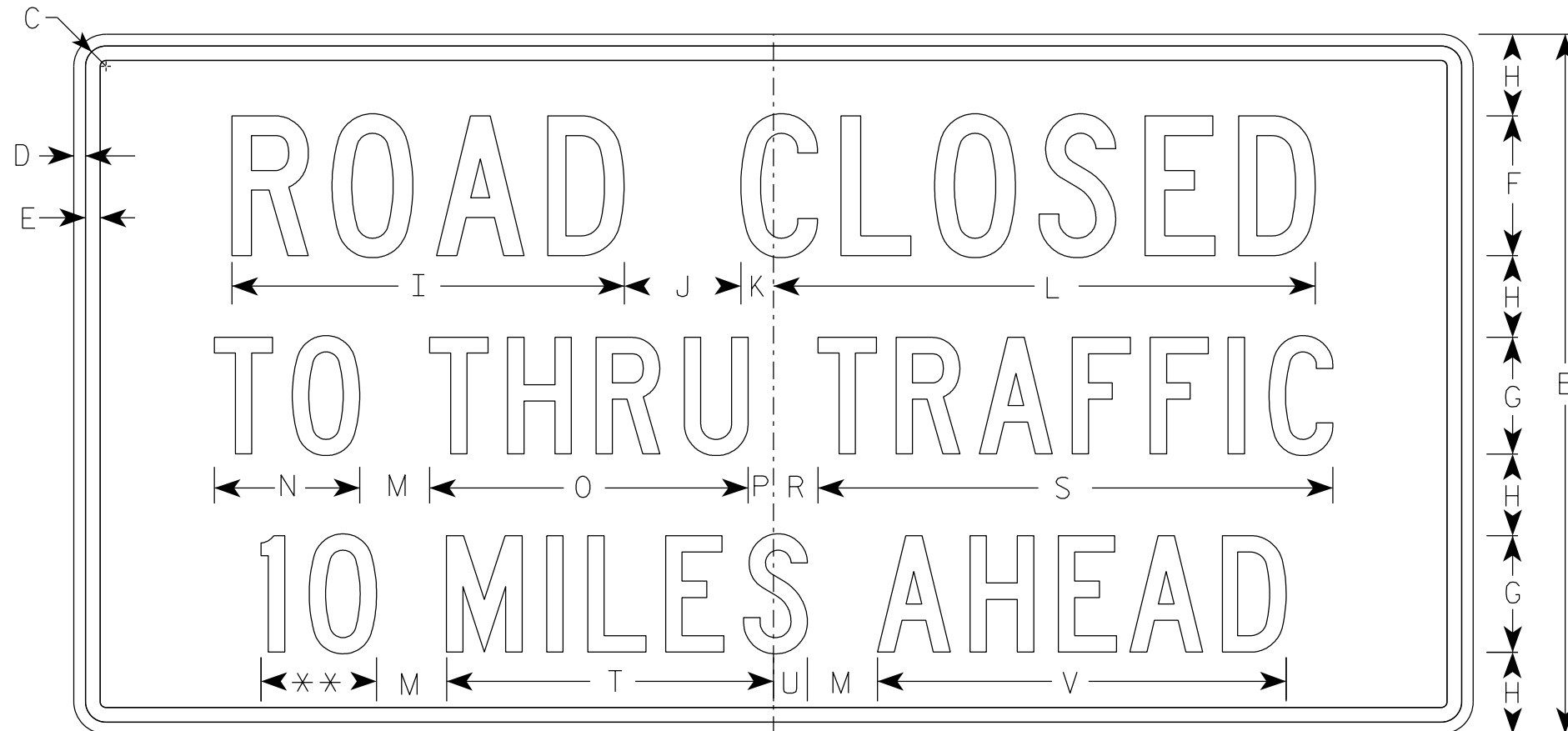
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 8/31/2020 PLATE NO. R9-9A.1

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

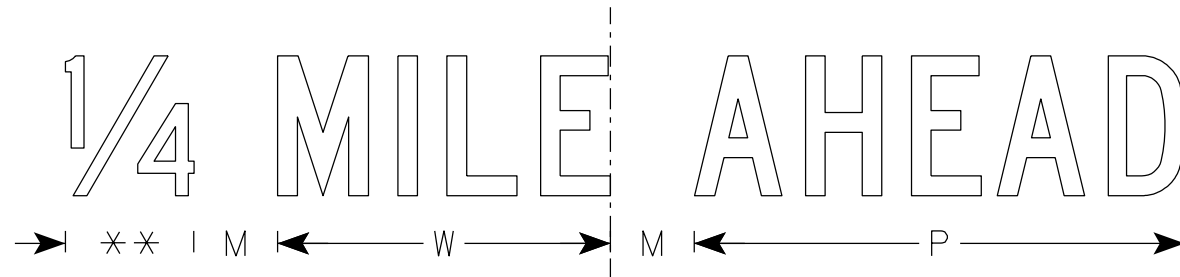
NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals to nearest quarter mile and optically adjust spacing to achieve proper balance.



R11-3

** See Note 5



| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | v | w | X | Y | Z | Area sq. ft. | |
|------|----|----|-------|-----|-----|---|---|-------|--------|---|-------|--------|---|-------|--------|-------|---|-------|--------|-------|-------|--------|--------|---|---|---|--------------|--|
| 1 | 36 | 18 | 1 1/4 | 3/8 | 3/8 | 4 | 3 | 2 | 11 1/4 | 3 | 1 1/8 | 15 3/8 | 2 | 3 3/4 | 8 1/4 | 5/8 | | 1 3/8 | 13 1/4 | 8 3/8 | 7/8 | 10 1/2 | 7 1/8 | | | | 4.5 | |
| 2S | 60 | 30 | 1 3/8 | 1/2 | 5/8 | 6 | 5 | 3 1/2 | 16 7/8 | 5 | 1 3/8 | 23 1/4 | 3 | 6 1/4 | 13 5/8 | 1 1/8 | | 1 7/8 | 22 1/8 | 14 | 1 1/2 | 17 1/2 | 11 7/8 | | | | 12.5 | |
| 2M | 60 | 30 | 1 3/8 | 1/2 | 5/8 | 6 | 5 | 3 1/2 | 16 7/8 | 5 | 1 3/8 | 23 1/4 | 3 | 6 1/4 | 13 5/8 | 1 1/8 | | 1 7/8 | 22 1/8 | 14 | 1 1/2 | 17 1/2 | 11 7/8 | | | | 12.5 | |
| 3 | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 4 | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 5 | | | | | | | | | | | | | | | | | | | | | | | | | | | | |

STANDARD SIGN
R11-3

WISCONSIN DEPT OF TRANSPORTATION

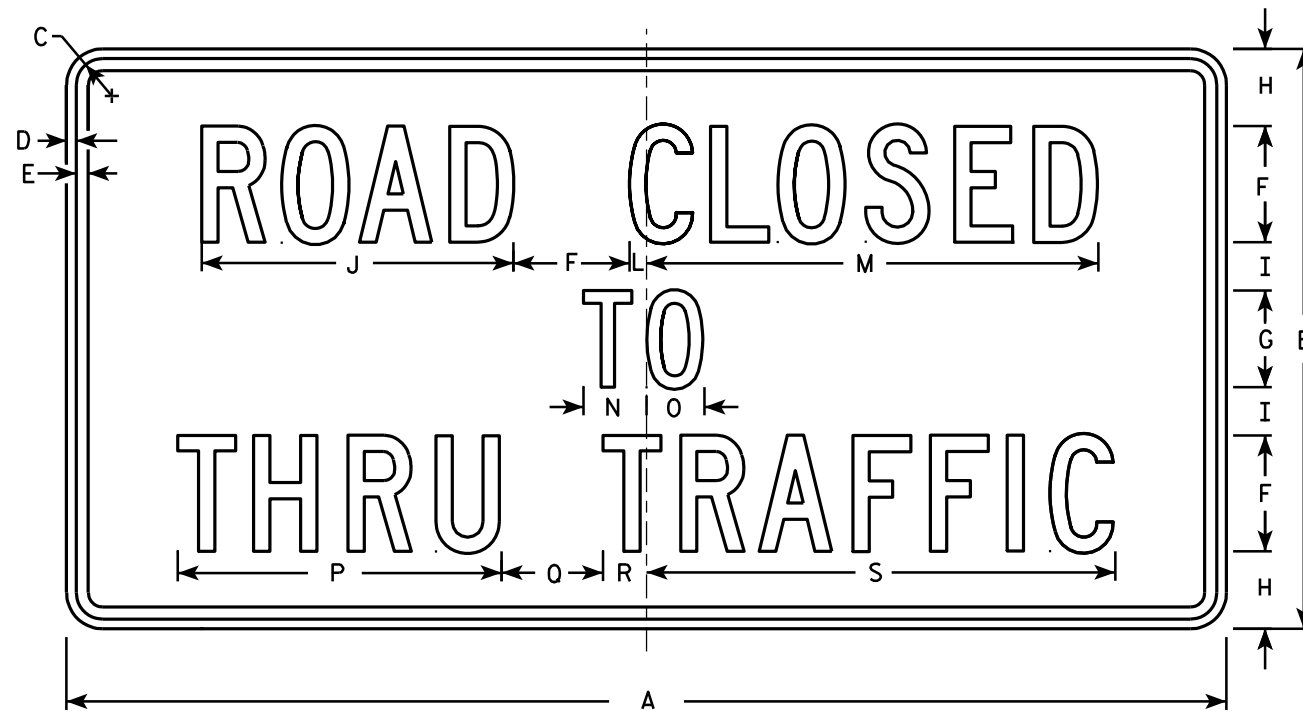
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 6/14/2021 PLATE NO. R11-3.9

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ **E**

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



R11-4

7

7

| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|----|-------|-----|-----|---|---|---|-------|--------|---|-----|--------|-------|---|--------|-------|-------|--------|---|---|---|---|---|---|---|-----------------|
| 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2S | 60 | 30 | 1 3/8 | 1/2 | 5/8 | 6 | 5 | 4 | 2 1/2 | 16 1/8 | | 7/8 | 23 3/8 | 3 1/4 | 3 | 16 3/4 | 5 1/4 | 2 1/4 | 24 1/4 | | | | | | | | 12.5 |
| 2M | 60 | 30 | 1 3/8 | 1/2 | 5/8 | 6 | 5 | 4 | 2 1/2 | 16 1/8 | | 7/8 | 23 3/8 | 3 1/4 | 3 | 16 3/4 | 5 1/4 | 2 1/4 | 24 1/4 | | | | | | | | 12.5 |
| 3 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 4 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 5 | | | | | | | | | | | | | | | | | | | | | | | | | | | |

STANDARD SIGN
R11 - 4

WISCONSIN DEPT OF TRANSPORTATION

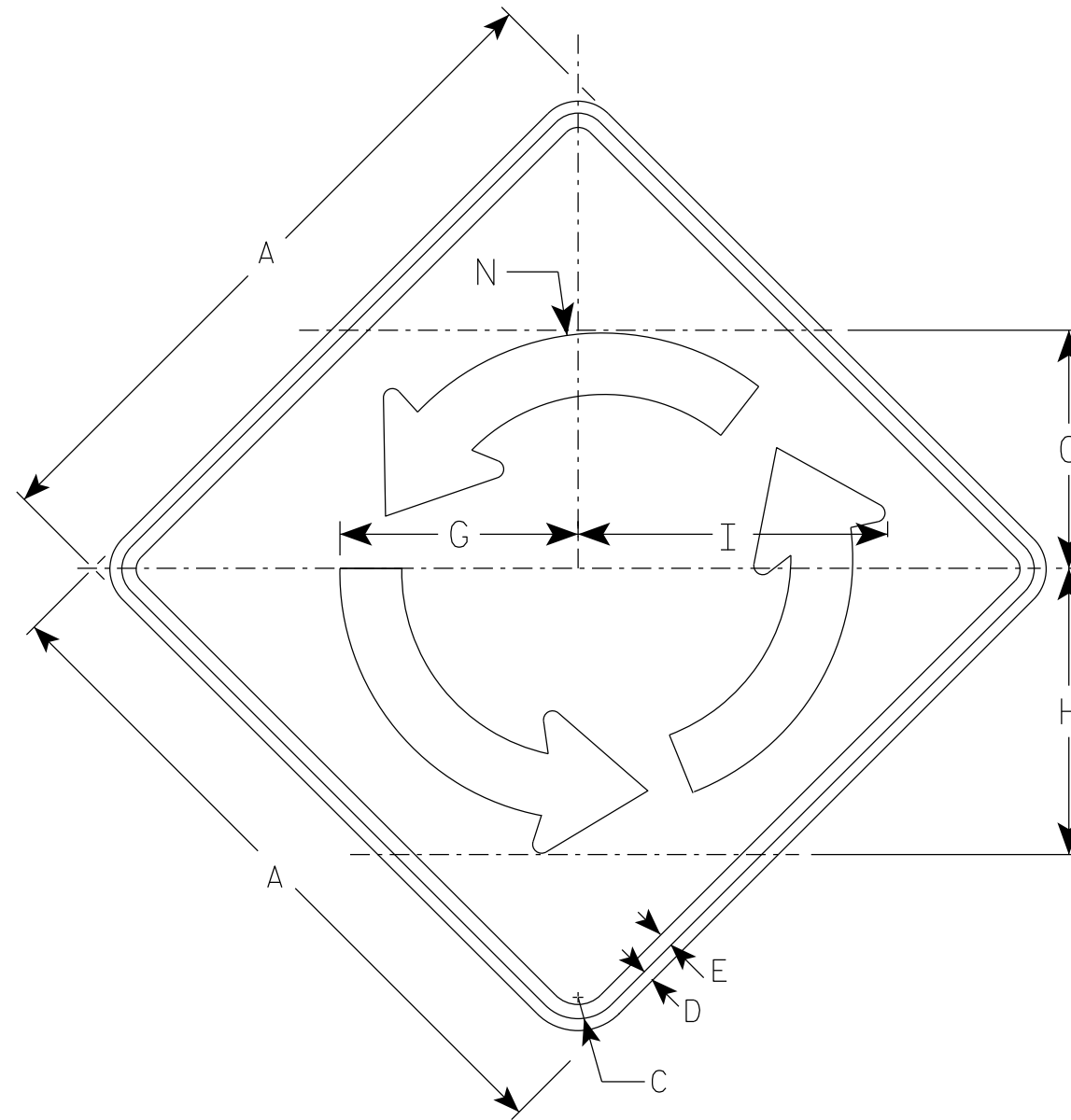
APPROVED *Matthew R. Raush*
for State Traffic Engineer

DATE 4/1/11 PLATE NO. R11-4.3

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E

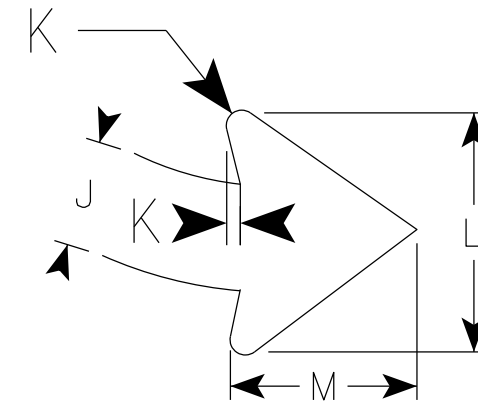
NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Yellow
Message - Black



W2-6

Arrow Detail



| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Area sq. ft. |
|------|----|---|-------|-----|-----|---|--------|--------|--------|-------|-----|-------|-------|--------|---|---|---|---|---|---|---|---|---|---|-----------------|
| 1 | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2S | 30 | | 1 3/8 | 1/2 | 5/8 | | 10 3/8 | 12 1/2 | 13 1/2 | 2 3/4 | 3/8 | 6 | 4 3/4 | 11 1/8 | | | | | | | | | | | 6.25 |
| 2M | 30 | | 1 3/8 | 1/2 | 5/8 | | 10 3/8 | 12 1/2 | 13 1/2 | 2 3/4 | 3/8 | 6 | 4 3/4 | 11 1/8 | | | | | | | | | | | 6.25 |
| 3 | 36 | | 1 5/8 | 5/8 | 3/4 | | 12 1/2 | 15 | 16 1/4 | 3 1/4 | 1/2 | 7 3/8 | 5 3/4 | 13 3/8 | | | | | | | | | | | 9.00 |
| 4 | 48 | | 2 1/4 | 3/4 | 1 | | 16 5/8 | 20 | 16 1/4 | 4 3/8 | 5/8 | 9 3/4 | 7 5/8 | 17 7/8 | | | | | | | | | | | 16.0 |
| 5 | | | | | | | | | | | | | | | | | | | | | | | | | |

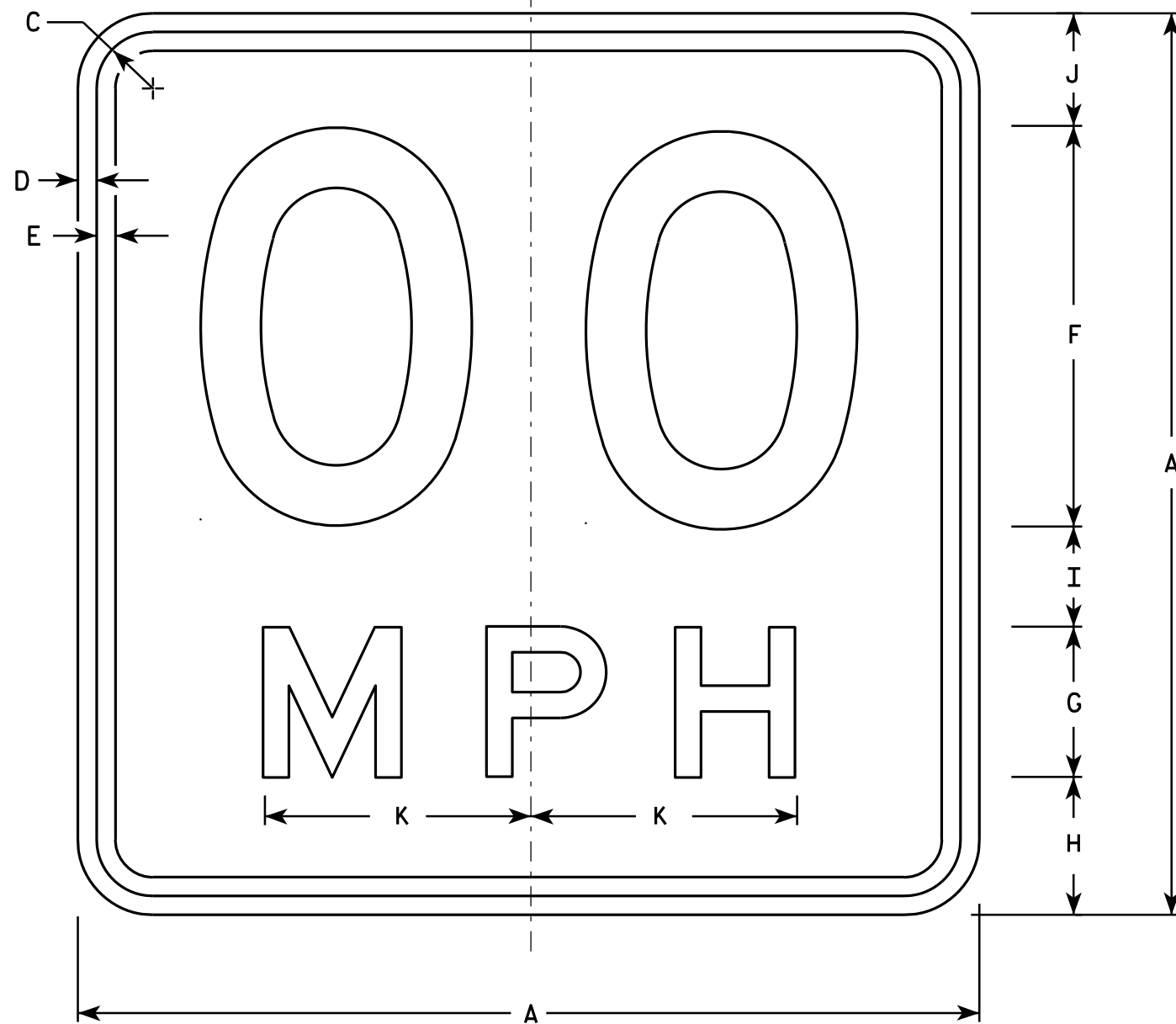
STANDARD SIGN
W2-6

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Rauch*
for State Traffic Engineer

DATE 3/24/21 PLATE NO. W2-6.7

PROJECT NO: SHEET NO: **E**



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Yellow
Message - Black
3. Message Series - See Note 6
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals and optically space about centerline to achieve proper balance.
6. Line 1 is Series D
Line 2 is Series E

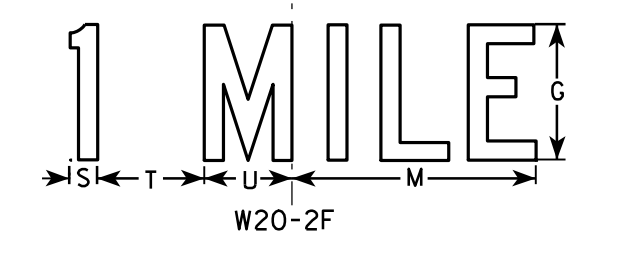
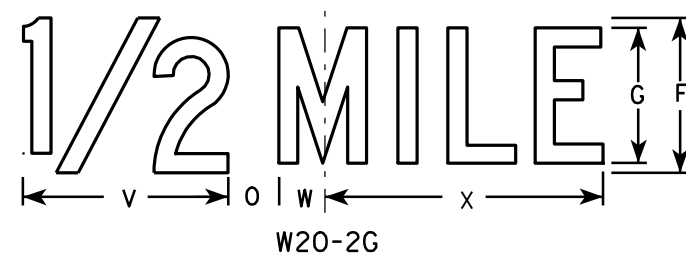
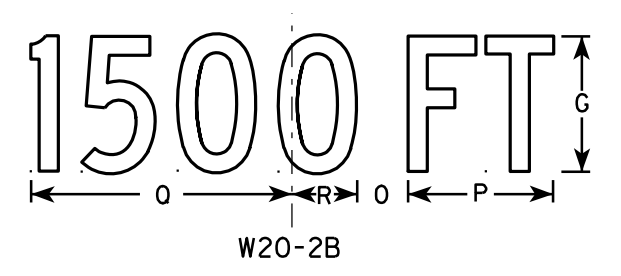
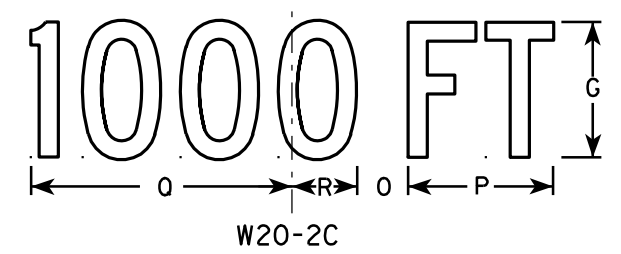
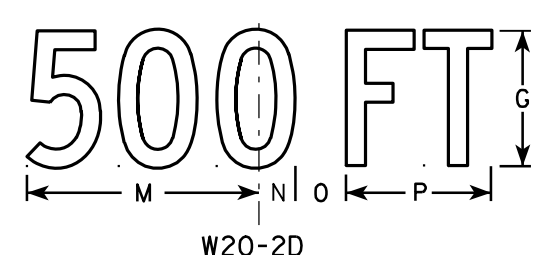
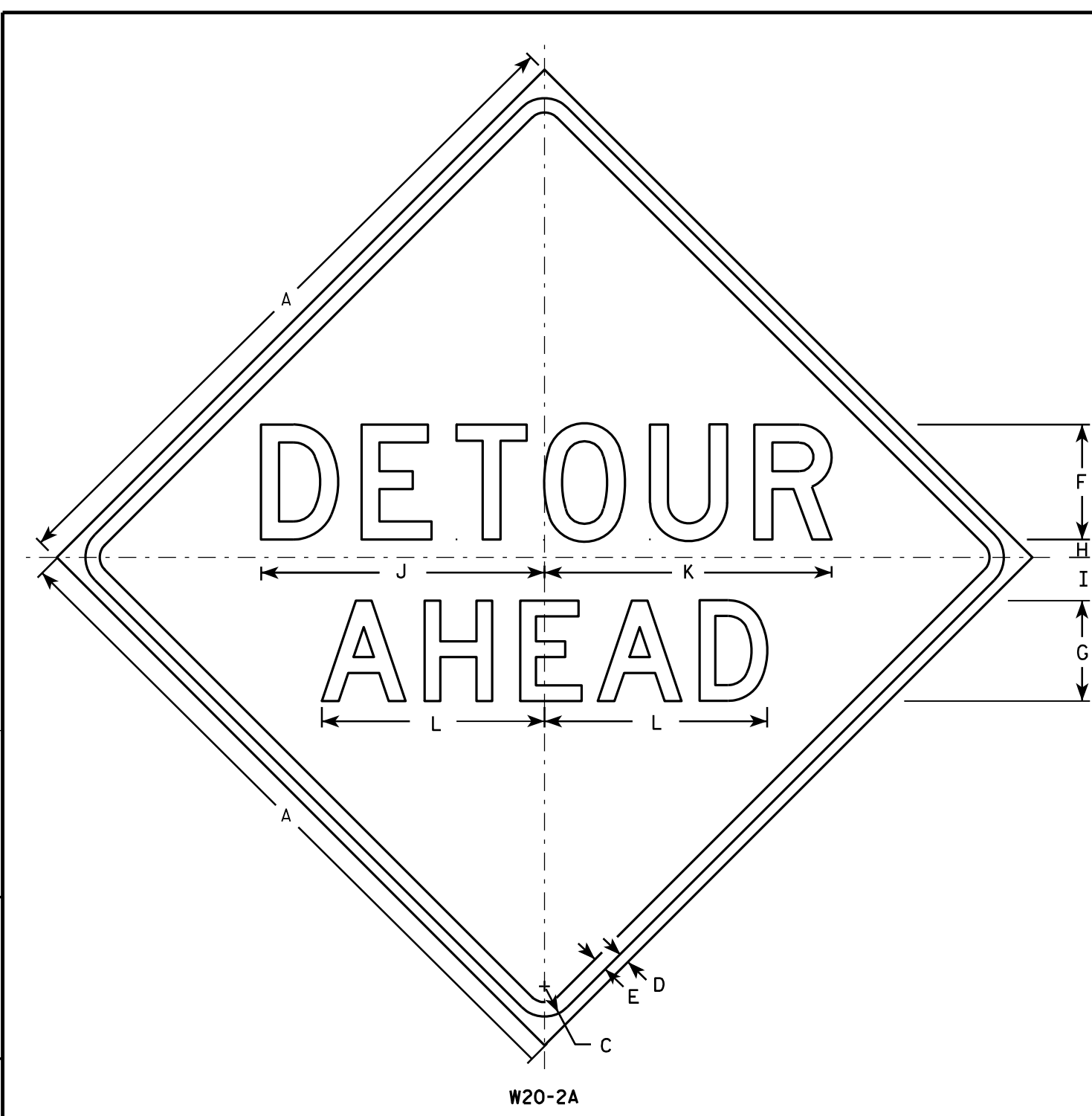
W13-1

* For 30" x 30" Warning Signs, use 18" x 18" W13-1 signs.
For 36" x 36" Warning Signs, use 24" x 24" W13-1 signs.

| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|---|-------|-----|-----|----|---|-------|-------|-------|--------|---|---|---|---|---|---|---|---|---|---|---|---|---|---|---|-----------------|
| 1 | 18 | | 1 1/8 | 3/8 | 3/8 | 8 | 3 | 2 3/4 | 2 | 2 1/4 | 5 3/8 | | | | | | | | | | | | | | | | 2.25 |
| * 2S | 18 | | 1 1/8 | 3/8 | 3/8 | 8 | 3 | 2 3/4 | 2 | 2 1/4 | 5 3/8 | | | | | | | | | | | | | | | | 2.25 |
| * 2M | 18 | | 1 1/8 | 3/8 | 3/8 | 8 | 3 | 2 3/4 | 2 | 2 1/4 | 5 3/8 | | | | | | | | | | | | | | | | 2.25 |
| 3 | 24 | | 1 1/8 | 3/8 | 1/2 | 10 | 4 | 4 | 2 3/4 | 3 1/4 | 6 5/8 | | | | | | | | | | | | | | | | 4.00 |
| 4 | 36 | | 1 5/8 | 5/8 | 3/4 | 16 | 6 | 5 1/2 | 4 | 4 1/2 | 10 5/8 | | | | | | | | | | | | | | | | 9.00 |
| 5 | 36 | | 1 5/8 | 5/8 | 3/4 | 16 | 6 | 5 1/2 | 4 | 4 1/2 | 10 5/8 | | | | | | | | | | | | | | | | 9.00 |

STANDARD SIGN
W13-1

WISCONSIN DEPT OF TRANSPORTATION
APPROVED *Matthew R. Rauch*
For State Traffic Engineer
DATE 5/31/12 PLATE NO. W13-1.16



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - See note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Line 1 is Series D.
Line 2 is Series D for AHEAD and Series C for all other distances.

7

7

| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|---|-------|-----|-----|---|---|-------|-------|--------|----|--------|----|-------|-------|-------|--------|-------|-------|-------|-------|--------|-------|--------|---|---|--------------|
| 1 | 36 | | 1 5/8 | 5/8 | 3/4 | 6 | 5 | 1 | 2 1/4 | 14 3/4 | 15 | 11 5/8 | 9 | 1 3/8 | 1 7/8 | 5 5/8 | 10 1/8 | 2 1/2 | 1 1/8 | 4 1/2 | 3 1/2 | 8 | 1 3/4 | 10 3/4 | | | 9.0 |
| 2S | 48 | | 2 1/4 | 3/4 | 1 | 8 | 7 | 1 1/4 | 3 | 19 3/4 | 20 | 15 1/2 | 12 | 1 7/8 | 2 5/8 | 7 1/2 | 13 1/2 | 3 3/8 | 1 1/2 | 6 | 4 5/8 | 10 5/8 | 2 3/8 | 14 3/8 | | | 16.0 |
| 2M | 48 | | 2 1/4 | 3/4 | 1 | 8 | 7 | 1 1/4 | 3 | 19 3/4 | 20 | 15 1/2 | 12 | 1 7/8 | 2 5/8 | 7 1/2 | 13 1/2 | 3 3/8 | 1 1/2 | 6 | 4 5/8 | 10 5/8 | 2 3/8 | 14 3/8 | | | 16.0 |
| 3 | 48 | | 2 1/4 | 3/4 | 1 | 8 | 7 | 1 1/4 | 3 | 19 3/4 | 20 | 15 1/2 | 12 | 1 7/8 | 2 5/8 | 7 1/2 | 13 1/2 | 3 3/8 | 1 1/2 | 6 | 4 5/8 | 10 5/8 | 2 3/8 | 14 3/8 | | | 16.0 |
| 4 | 48 | | 2 1/4 | 3/4 | 1 | 8 | 7 | 1 1/4 | 3 | 19 3/4 | 20 | 15 1/2 | 12 | 1 7/8 | 2 5/8 | 7 1/2 | 13 1/2 | 3 3/8 | 1 1/2 | 6 | 4 5/8 | 10 5/8 | 2 3/8 | 14 3/8 | | | 16.0 |
| 5 | 48 | | 2 1/4 | 3/4 | 1 | 8 | 7 | 1 1/4 | 3 | 19 3/4 | 20 | 15 1/2 | 12 | 1 7/8 | 2 5/8 | 7 1/2 | 13 1/2 | 3 3/8 | 1 1/2 | 6 | 4 5/8 | 10 5/8 | 2 3/8 | 14 3/8 | | | 16.0 |

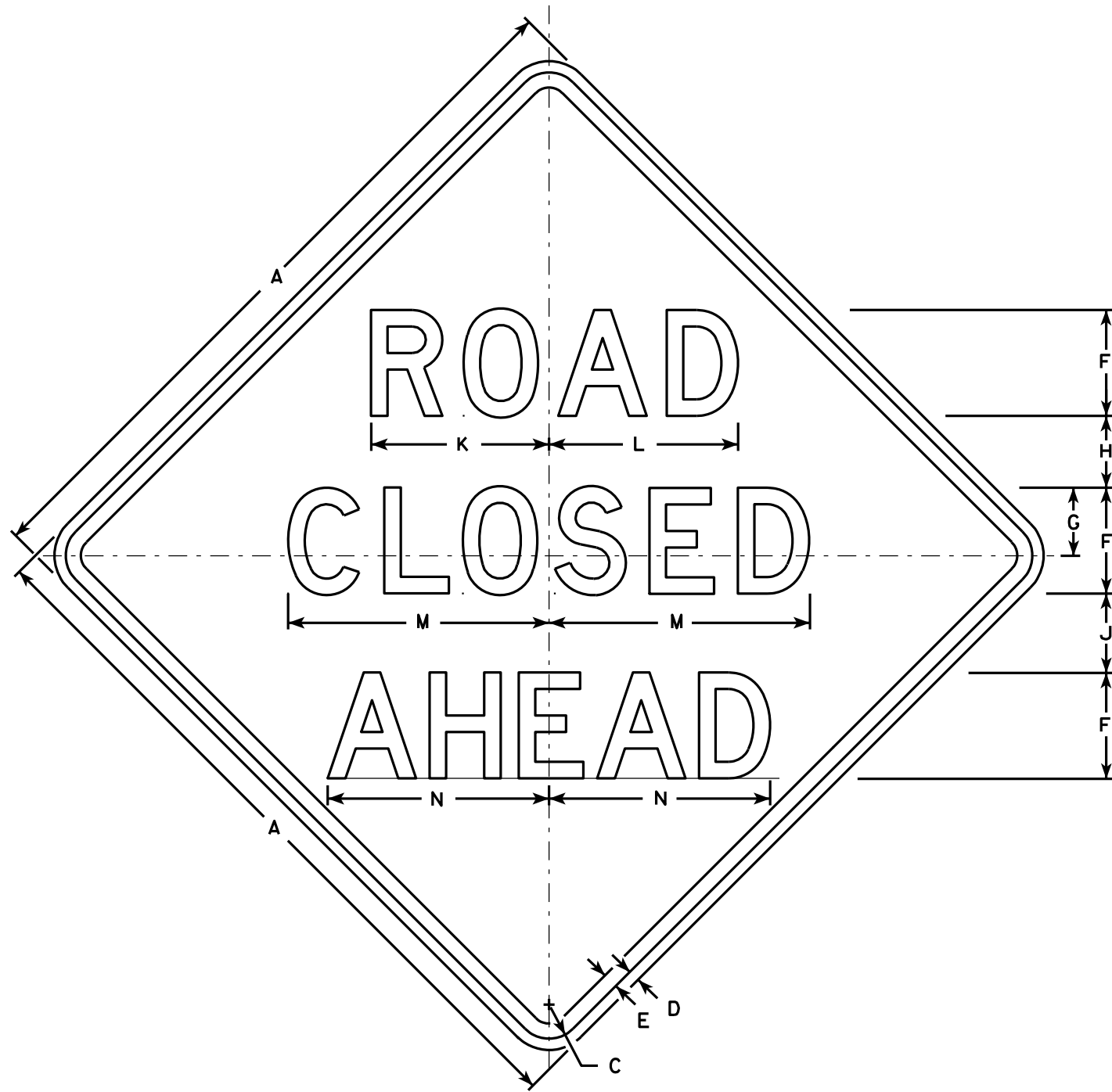
STANDARD SIGN
W20-2A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

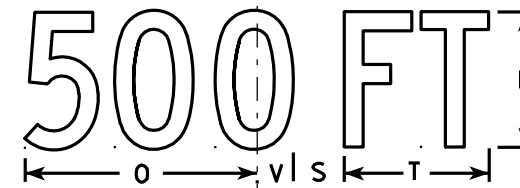
APPROVED *Matthew R. Raub*
for State Traffic Engineer

DATE 3/18/11 PLATE NO. W20-2.6

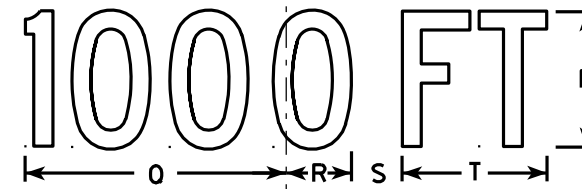
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E



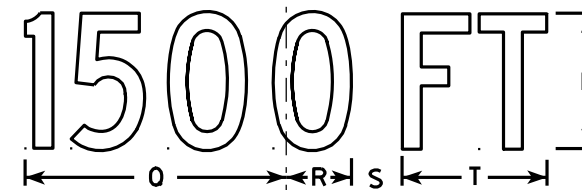
W20-3A



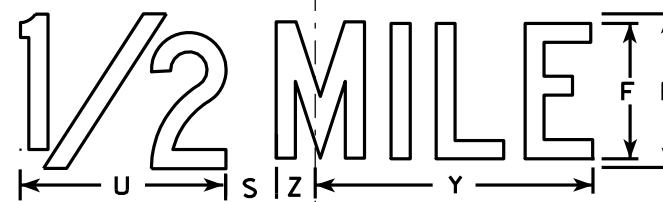
W20-3D



W20-3C



W20-3B



W20-3G



W20-3F

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - see note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Lines 1 and 2 are Series D.
Line 3 is Series D for AHEAD and Series C for all other distances.

7

7

| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|---|-------|-----|-----|---|-------|-------|-------|-------|--------|--------|--------|--------|----|---|--------|-------|-------|-------|--------|-------|-------|-------|--------|-------|--------------|
| 1 | 36 | | 1 5/8 | 5/8 | 3/4 | 5 | 3 3/8 | 3 1/2 | 1 1/8 | 4 | 8 3/8 | 8 7/8 | 12 1/2 | 11 | 9 | 6 | 10 1/8 | 2 1/2 | 1 7/8 | 5 5/8 | 8 | 1 3/8 | 4 1/2 | 3 1/2 | 10 3/4 | 1 3/4 | 9.0 |
| 2S | 48 | | 2 1/4 | 3/4 | 1 | 7 | 4 1/2 | 4 3/4 | 1 1/2 | 5 1/4 | 11 3/4 | 12 1/2 | 17 1/4 | 14 5/8 | 12 | 8 | 13 1/2 | 3 3/8 | 2 5/8 | 7 1/2 | 10 5/8 | 1 7/8 | 6 | 4 5/8 | 14 3/8 | 2 3/8 | 16.0 |
| 2M | 48 | | 2 1/4 | 3/4 | 1 | 7 | 4 1/2 | 4 3/4 | 1 1/2 | 5 1/4 | 11 3/4 | 12 1/2 | 17 1/4 | 14 5/8 | 12 | 8 | 13 1/2 | 3 3/8 | 2 5/8 | 7 1/2 | 10 5/8 | 1 7/8 | 6 | 4 5/8 | 14 3/8 | 2 3/8 | 16.0 |
| 3 | 48 | | 2 1/4 | 3/4 | 1 | 7 | 4 1/2 | 4 3/4 | 1 1/2 | 5 1/4 | 11 3/4 | 12 1/2 | 17 1/4 | 14 5/8 | 12 | 8 | 13 1/2 | 3 3/8 | 2 5/8 | 7 1/2 | 10 5/8 | 1 7/8 | 6 | 4 5/8 | 14 3/8 | 2 3/8 | 16.0 |
| 4 | 48 | | 2 1/4 | 3/4 | 1 | 7 | 4 1/2 | 4 3/4 | 1 1/2 | 5 1/4 | 11 3/4 | 12 1/2 | 17 1/4 | 14 5/8 | 12 | 8 | 13 1/2 | 3 3/8 | 2 5/8 | 7 1/2 | 10 5/8 | 1 7/8 | 6 | 4 5/8 | 14 3/8 | 2 3/8 | 16.0 |
| 5 | 48 | | 2 1/4 | 3/4 | 1 | 7 | 4 1/2 | 4 3/4 | 1 1/2 | 5 1/4 | 11 3/4 | 12 1/2 | 17 1/4 | 14 5/8 | 12 | 8 | 13 1/2 | 3 3/8 | 2 5/8 | 7 1/2 | 10 5/8 | 1 7/8 | 6 | 4 5/8 | 14 3/8 | 2 3/8 | 16.0 |

STANDARD SIGN
W20-3A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 3/18/11 PLATE NO. W20-3.7

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E

CENTER ISLAND

| STATION | REAL STATION | DISTANCE | AREA (SF) | | INCREMENTAL VOL (CY) (UNADJUSTED) | | CUMULATIVE VOL (CY) | | |
|---------|--------------|----------|-----------|------|-----------------------------------|------|---------------------|---------------|---------------|
| | | | CUT | FILL | CUT | FILL | CUT | EXPANDED FILL | MASS ORDINATE |
| | | | | | | | | | |
| 10+00 | 1000.00 | 0.00 | 32.56 | 0.00 | 0 | 0 | 0 | 0 | 0 |
| 10+16 | 1016.00 | 16.00 | 44.64 | 0.35 | 23 | 0 | 23 | 0 | 23 |
| 10+25 | 1025.00 | 9.00 | 35.34 | 0.00 | 13 | 0 | 36 | 0 | 36 |
| 10+50 | 1050.00 | 25.00 | 43.83 | 0.00 | 37 | 0 | 73 | 0 | 73 |
| 10+55 | 1055.00 | 5.00 | 59.86 | 0.00 | 10 | 0 | 83 | 0 | 83 |
| 10+75 | 1075.00 | 20.00 | 50.68 | 0.00 | 41 | 0 | 124 | 0 | 124 |
| 10+90 | 1090.00 | 15.00 | 48.88 | 0.06 | 28 | 0 | 152 | 0 | 152 |
| 11+00 | 1100.00 | 10.00 | 45.83 | 0.00 | 18 | 0 | 170 | 0 | 170 |
| 11+25 | 1125.00 | 25.00 | 45.71 | 0.05 | 42 | 0 | 212 | 0 | 212 |

EAST AVE-NB ENTRY

| STATION | REAL STATION | DISTANCE | AREA (SF) | | INCREMENTAL VOL (CY) (UNADJUSTED) | | CUMULATIVE VOL (CY) | | |
|------------|--------------|----------|-----------|------|-----------------------------------|------|---------------------|---------------|---------------|
| | | | CUT | FILL | CUT | FILL | CUT | EXPANDED FILL | MASS ORDINATE |
| | | | | | | | | | |
| 500+14.007 | 50014.01 | 0.00 | 27.01 | 0.00 | 0 | 0 | 0 | 0 | 0 |
| 500+25 | 50025.00 | 10.99 | 31.78 | 0.61 | 12 | 0 | 12 | 0 | 12 |
| 500+50 | 50050.00 | 25.00 | 25.86 | 2.75 | 27 | 2 | 39 | 3 | 36 |
| 500+52.914 | 50052.91 | 2.91 | 25.19 | 3.18 | 3 | 0 | 42 | 3 | 39 |
| 500+75 | 50075.00 | 22.09 | 23.14 | 7.68 | 20 | 4 | 62 | 8 | 54 |
| 500+88.045 | 50088.05 | 13.05 | 17.99 | 8.74 | 10 | 4 | 72 | 13 | 59 |

EAST AVE-NB EXIT

| STATION | REAL STATION | DISTANCE | AREA (SF) | | INCREMENTAL VOL (CY) (UNADJUSTED) | | CUMULATIVE VOL (CY) | | |
|------------|--------------|----------|-----------|------|-----------------------------------|------|---------------------|---------------|---------------|
| | | | CUT | FILL | CUT | FILL | CUT | EXPANDED FILL | MASS ORDINATE |
| | | | | | | | | | |
| 501+72.176 | 50172.18 | 0.00 | 31.71 | 0.02 | 0 | 0 | 0 | 0 | 0 |
| 502+00 | 50200.00 | 27.82 | 36.38 | 0.32 | 35 | 0 | 35 | 0 | 35 |
| 502+25 | 50225.00 | 25.00 | 30.03 | 0.00 | 31 | 0 | 66 | 0 | 66 |
| 502+37.793 | 50237.79 | 12.79 | 26.77 | 0.00 | 13 | 0 | 79 | 0 | 79 |
| 502+50 | 50250.00 | 12.21 | 27.69 | 0.00 | 12 | 0 | 91 | 0 | 91 |
| 502+75 | 50275.00 | 25.00 | 29.71 | 0.00 | 27 | 0 | 118 | 0 | 118 |
| 502+89.747 | 50289.75 | 14.75 | 30.22 | 0.00 | 16 | 0 | 134 | 0 | 134 |

EAST AVE-SB ENTRY

| STATION | REAL STATION | DISTANCE | AREA (SF) | | INCREMENTAL VOL (CY) (UNADJUSTED) | | CUMULATIVE VOL (CY) | | |
|------------|--------------|----------|-----------|------|-----------------------------------|------|---------------------|---------------|---------------|
| | | | CUT | FILL | CUT | FILL | CUT | EXPANDED FILL | MASS ORDINATE |
| | | | | | | | | | |
| 501+67.091 | 50167.09 | 0.00 | 28.37 | 1.16 | 0 | 0 | 0 | 0 | 0 |
| 501+75 | 50175.00 | 7.91 | 34.06 | 2.19 | 9 | 0 | 9 | 0 | 9 |
| 501+82.051 | 50182.05 | 7.05 | 32.53 | 2.86 | 9 | 1 | 18 | 1 | 17 |
| 501+97.011 | 50197.01 | 14.96 | 31.14 | 3.18 | 18 | 2 | 36 | 4 | 32 |
| 502+00 | 50200.00 | 2.99 | 31.86 | 2.80 | 3 | 0 | 39 | 4 | 35 |
| 502+25 | 50225.00 | 25.00 | 30.41 | 0.00 | 29 | 1 | 68 | 5 | 63 |
| 502+44.545 | 50244.54 | 19.54 | 29.35 | 0.00 | 22 | 0 | 90 | 5 | 85 |
| 502+50 | 50250.00 | 5.46 | 29.20 | 0.00 | 6 | 0 | 96 | 5 | 91 |
| 502+75 | 50275.00 | 25.00 | 29.14 | 0.00 | 27 | 0 | 123 | 5 | 118 |
| 502+90 | 50290.00 | 15.00 | 28.99 | 0.00 | 16 | 0 | 139 | 5 | 134 |

EAST AVE-SB EXIT

| STATION | REAL STATION | DISTANCE | AREA (SF) | | INCREMENTAL VOL (CY) (UNADJUSTED) | | CUMULATIVE VOL (CY) | | |
|------------|--------------|----------|-----------|------|-----------------------------------|------|---------------------|---------------|---------------|
| | | | CUT | FILL | CUT | FILL | CUT | EXPANDED FILL | MASS ORDINATE |
| | | | | | | | | | |
| 500+14 | 50014.00 | 0.00 | 27.35 | 0.00 | 0 | 0 | 0 | 0 | 0 |
| 500+20.138 | 50020.14 | 6.14 | 26.70 | 0.00 | 6 | 0 | 6 | 0 | 6 |
| 500+25 | 50025.00 | 4.86 | 26.24 | 0.00 | 5 | 0 | 11 | 0 | 11 |
| 500+50 | 50050.00 | 25.00 | 29.97 | 0.40 | 26 | 0 | 37 | 0 | 37 |
| 500+61.748 | 50061.75 | 11.75 | 29.18 | 1.06 | 13 | 0 | 50 | 0 | 50 |
| 500+75 | 50075.00 | 13.25 | 40.56 | 0.00 | 17 | 0 | 67 | 0 | 67 |
| 500+76.436 | 50076.44 | 1.44 | 40.46 | 0.00 | 2 | 0 | 69 | 0 | 69 |
| 500+91.124 | 50091.12 | 14.69 | 37.46 | 0.07 | 21 | 0 | 90 | 0 | 90 |

RIDERS CLUB-EB ENTRY

| STATION | REAL STATION | DISTANCE | AREA (SF) | | INCREMENTAL VOL (CY) (UNADJUSTED) | | CUMULATIVE VOL (CY) | | |
|-----------|--------------|----------|-----------|------|-----------------------------------|------|---------------------|---------------|---------------|
| | | | CUT | FILL | CUT | FILL | CUT | EXPANDED FILL | MASS ORDINATE |
| | | | | | | | | | |
| 34+85.174 | 3485.17 | 0.00 | 28.84 | 0.00 | 0 | 0 | 0 | 0 | 0 |
| 35+00 | 3500.00 | 14.83 | 30.29 | 0.00 | 16 | 0 | 16 | 0 | 16 |
| 35+25 | 3525.00 | 25.00 | 29.31 | 0.00 | 28 | 0 | 44 | 0 | 44 |
| 35+50 | 3550.00 | 25.00 | 35.35 | 1.36 | 30 | 1 | 74 | 1 | 73 |
| 35+64.774 | 3564.77 | 14.77 | 33.31 | 0.74 | 19 | 1 | 93 | 3 | 90 |
| 35+75 | 3575.00 | 10.23 | 33.31 | 0.59 | 13 | 0 | 106 | 3 | 103 |
| 35+91.649 | 3591.65 | 16.65 | 30.18 | 0.06 | 20 | 0 | 126 | 3 | 123 |

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RIDERS CLUB-EB EXIT

| STATION | REAL STATION | DISTANCE | AREA (SF) | | INCREMENTAL VOL (CY) (UNADJUSTED) | | CUMULATIVE VOL (CY) | | |
|-----------|--------------|----------|-----------|------|-----------------------------------|--------|---------------------|---------------|---------------|
| | | | CUT | FILL | CUT | FILL | CUT | EXPANDED FILL | MASS ORDINATE |
| | | | | | NOTE 1 | NOTE 3 | NOTE 1 | 1.30 | NOTE 8 |
| 36+72.564 | 3672.56 | 0.00 | 29.13 | 1.62 | 0 | 0 | 0 | 0 | 0 |
| 36+91.36 | 3691.36 | 18.80 | 33.55 | 1.53 | 22 | 1 | 22 | 1 | 21 |
| 37+00 | 3700.00 | 8.64 | 28.52 | 0.00 | 10 | 0 | 32 | 1 | 31 |
| 37+16.45 | 3716.45 | 16.45 | 29.28 | 0.00 | 18 | 0 | 50 | 1 | 49 |
| 37+25 | 3725.00 | 8.55 | 29.16 | 0.00 | 9 | 0 | 59 | 1 | 58 |
| 37+38.247 | 3738.25 | 13.25 | 28.31 | 0.00 | 14 | 0 | 73 | 1 | 72 |
| 37+49.769 | 3749.77 | 11.52 | 27.51 | 0.00 | 12 | 0 | 85 | 1 | 84 |

DIVISION 1 - RIDERS CLUB-WB ENTRY

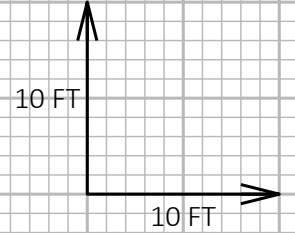
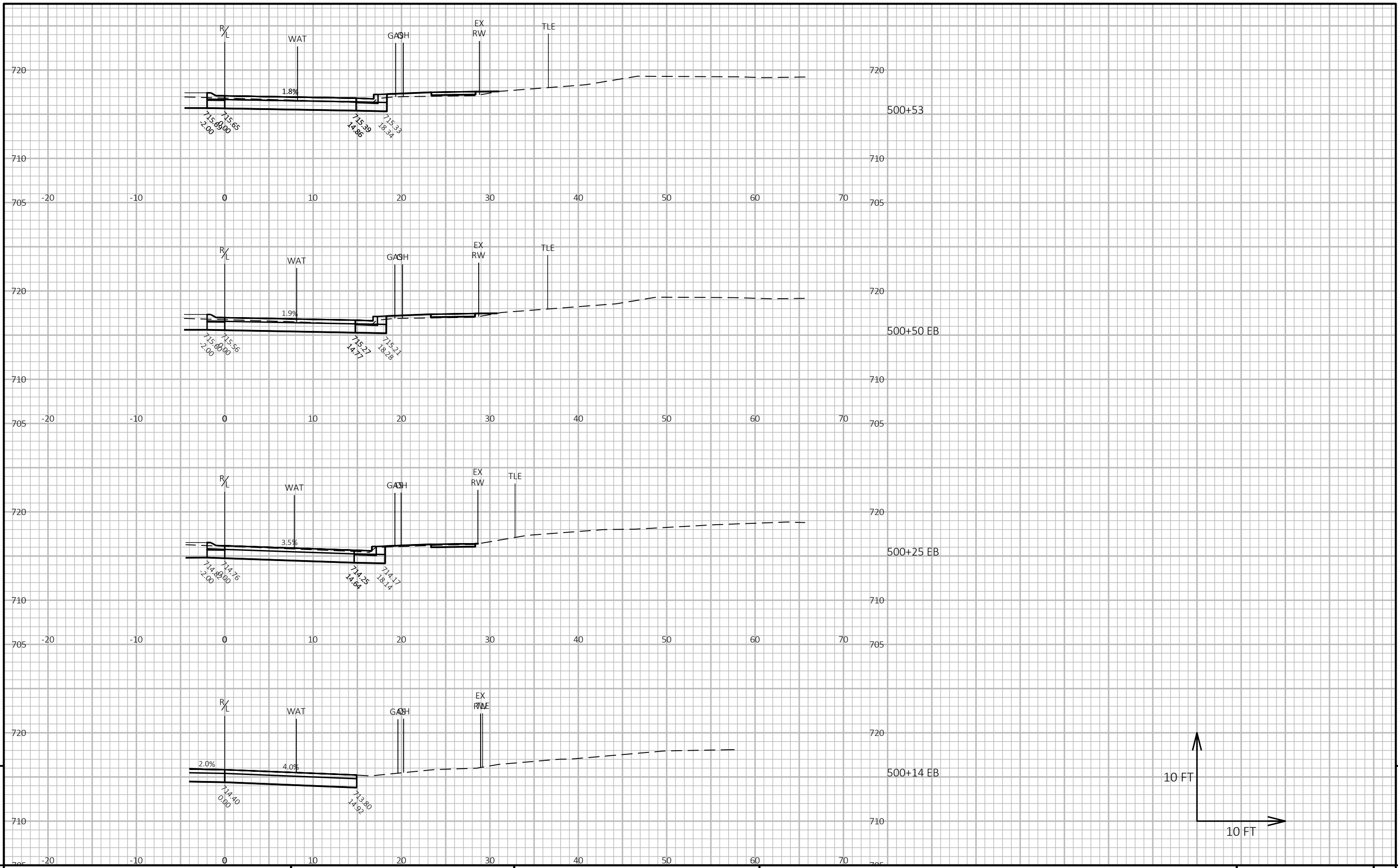
| STATION | REAL STATION | DISTANCE | AREA (SF) | | INCREMENTAL VOL (CY) (UNADJUSTED) | | CUMULATIVE VOL (CY) | | |
|-----------|--------------|----------|-----------|------|-----------------------------------|--------|---------------------|---------------|---------------|
| | | | CUT | FILL | CUT | FILL | CUT | EXPANDED FILL | MASS ORDINATE |
| | | | | | NOTE 1 | NOTE 3 | NOTE 1 | 1.30 | NOTE 8 |
| 36+67.641 | 3667.64 | 0.00 | 33.15 | 0.00 | 0 | 0 | 0 | 0 | 0 |
| 36+83.061 | 3683.06 | 15.42 | 28.26 | 2.43 | 18 | 1 | 18 | 1 | 17 |
| 36+98.482 | 3698.48 | 15.42 | 29.57 | 2.68 | 17 | 1 | 35 | 3 | 32 |
| 37+00 | 3700.00 | 1.52 | 30.14 | 2.43 | 2 | 0 | 37 | 3 | 34 |
| 37+19.505 | 3719.51 | 19.51 | 29.55 | 0.00 | 22 | 1 | 59 | 4 | 55 |
| 37+25 | 3725.00 | 5.49 | 29.42 | 0.00 | 6 | 0 | 65 | 4 | 61 |
| 37+40.529 | 3740.53 | 15.53 | 27.95 | 0.00 | 16 | 0 | 81 | 4 | 77 |
| 37+50 | 3750.00 | 9.47 | 26.90 | 0.00 | 10 | 0 | 91 | 4 | 87 |

RIDERS CLUB-WB EXIT

| STATION | REAL STATION | DISTANCE | AREA (SF) | | INCREMENTAL VOL (CY) (UNADJUSTED) | | CUMULATIVE VOL (CY) | | |
|-----------|--------------|----------|-----------|------|-----------------------------------|--------|---------------------|---------------|---------------|
| | | | CUT | FILL | CUT | FILL | CUT | EXPANDED FILL | MASS ORDINATE |
| | | | | | NOTE 1 | NOTE 3 | NOTE 1 | 1.30 | NOTE 8 |
| 34+85 | 3485.00 | 0.00 | 25.16 | 0.00 | 0 | 0 | 0 | 0 | 0 |
| 35+00 | 3500.00 | 15.00 | 26.77 | 0.00 | 14 | 0 | 14 | 0 | 14 |
| 35+20.619 | 3520.62 | 20.62 | 27.09 | 0.00 | 21 | 0 | 35 | 0 | 35 |
| 35+25 | 3525.00 | 4.38 | 27.08 | 0.00 | 4 | 0 | 39 | 0 | 39 |
| 35+50 | 3550.00 | 25.00 | 27.14 | 0.00 | 25 | 0 | 64 | 0 | 64 |
| 35+59.099 | 3559.10 | 9.10 | 33.88 | 0.13 | 10 | 0 | 74 | 0 | 74 |
| 35+75 | 3575.00 | 15.90 | 32.97 | 0.77 | 20 | 0 | 94 | 0 | 94 |
| 35+75.552 | 3575.55 | 0.55 | 32.95 | 0.82 | 1 | 0 | 95 | 0 | 95 |
| 35+92.005 | 3592.00 | 16.45 | 41.03 | 0.02 | 23 | 0 | 118 | 0 | 118 |

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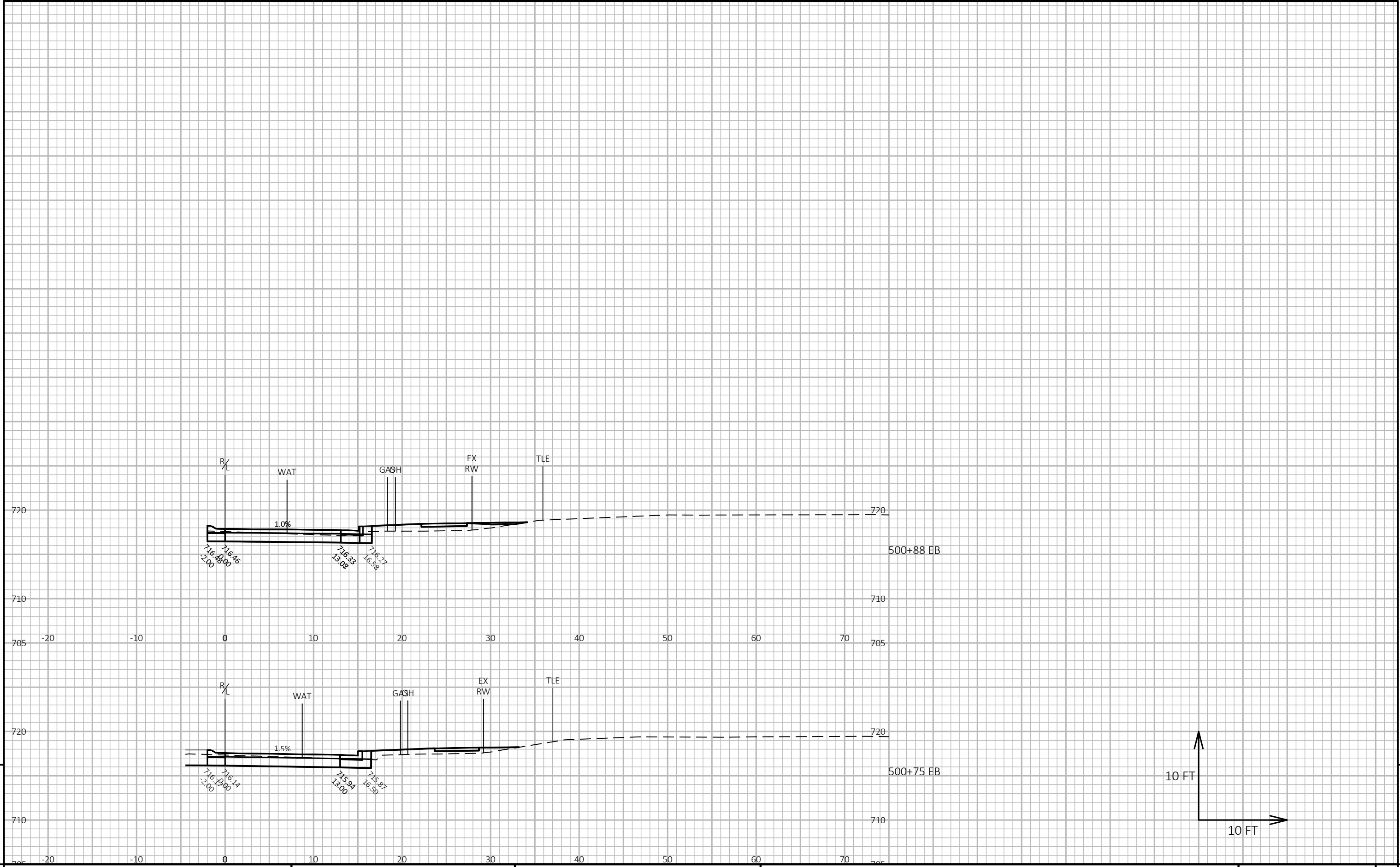
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PROJECT NO: 5991-02-62 HWY: EAST AVENUE N COUNTY: LA CROSSE CROSS SECTIONS: EAST AVENUE N SHEET E

FILE NAME: X:\KO\O\ONALA\160943\5-FINAL-DSGN\51-DRAWINGS\40-TRANSHWY\CIVIL 3D\59910261\SHEETS\PLAN\SEC 09 B CROSS SECTIONS\090201-XS (NORTHBOUND).DWG PLOT DATE: 9/1/2022 9:53 AM PLOT BY: CLARISSA ERBS PLOT NAME: PLOT SCALE: 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49

LAYOUT NAME - 01



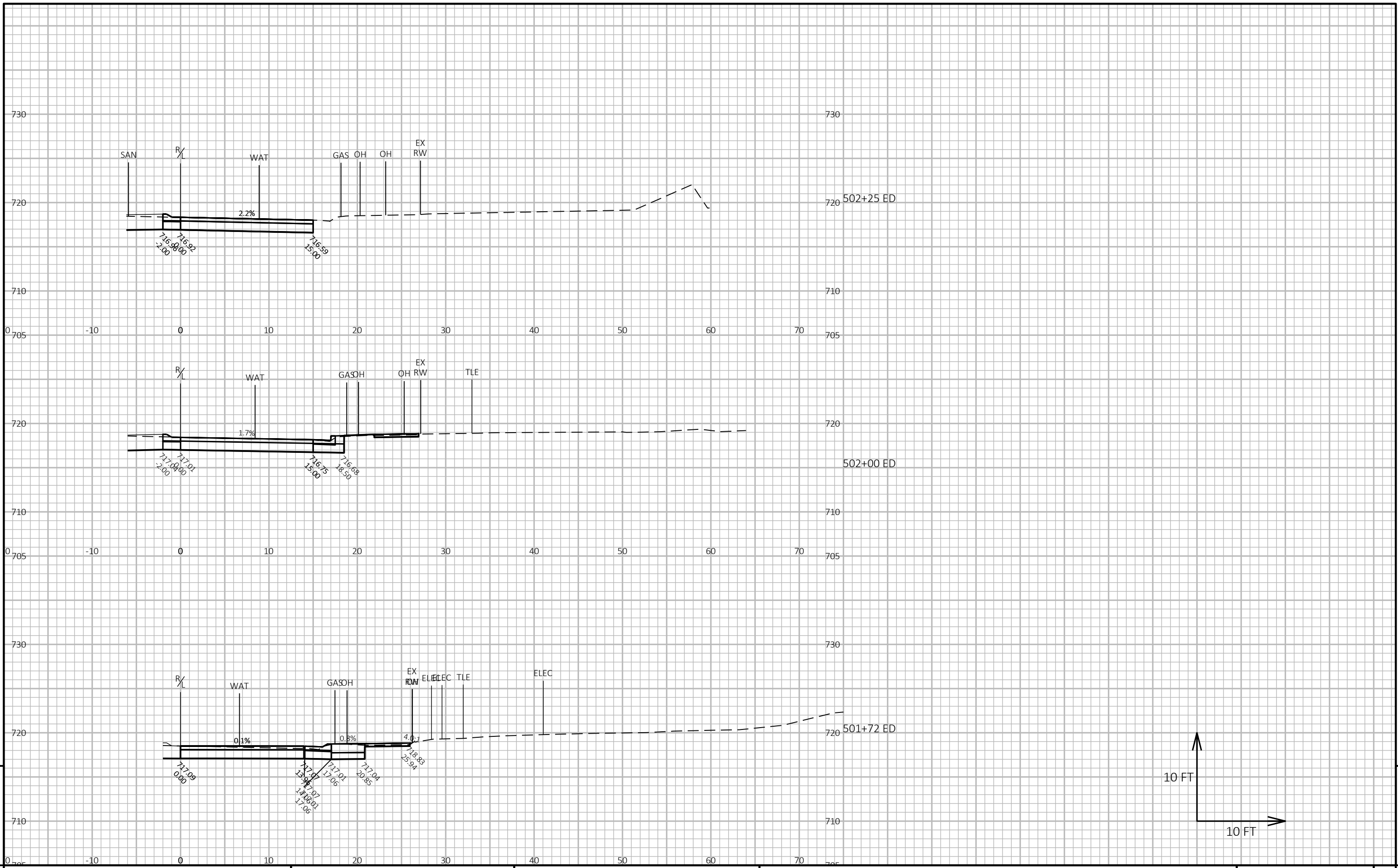
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| | | | | | |
|------------------------|--------------------|-------------------|-------------------------------|-------|---|
| PROJECT NO: 5991-02-62 | HWY: EAST AVENUE N | COUNTY: LA CROSSE | CROSS SECTIONS: EAST AVENUE N | SHEET | E |
|------------------------|--------------------|-------------------|-------------------------------|-------|---|

FILE NAME : X:\KO\O\ONALA\160943\5-FINAL-DSGN\51-DRAWINGS\40-TRANSHWY\CIVIL 3D\59910261\SHEETSPLAN\SEC 09 B CROSS SECTIONS\090201-XS (NORTHBOUND).DWG PLOT DATE : 9/1/2022 9:53 AM PLOT BY : CLARISSA ERBS PLOT NAME : PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADDs SHEET 49

LAYOUT NAME - 02



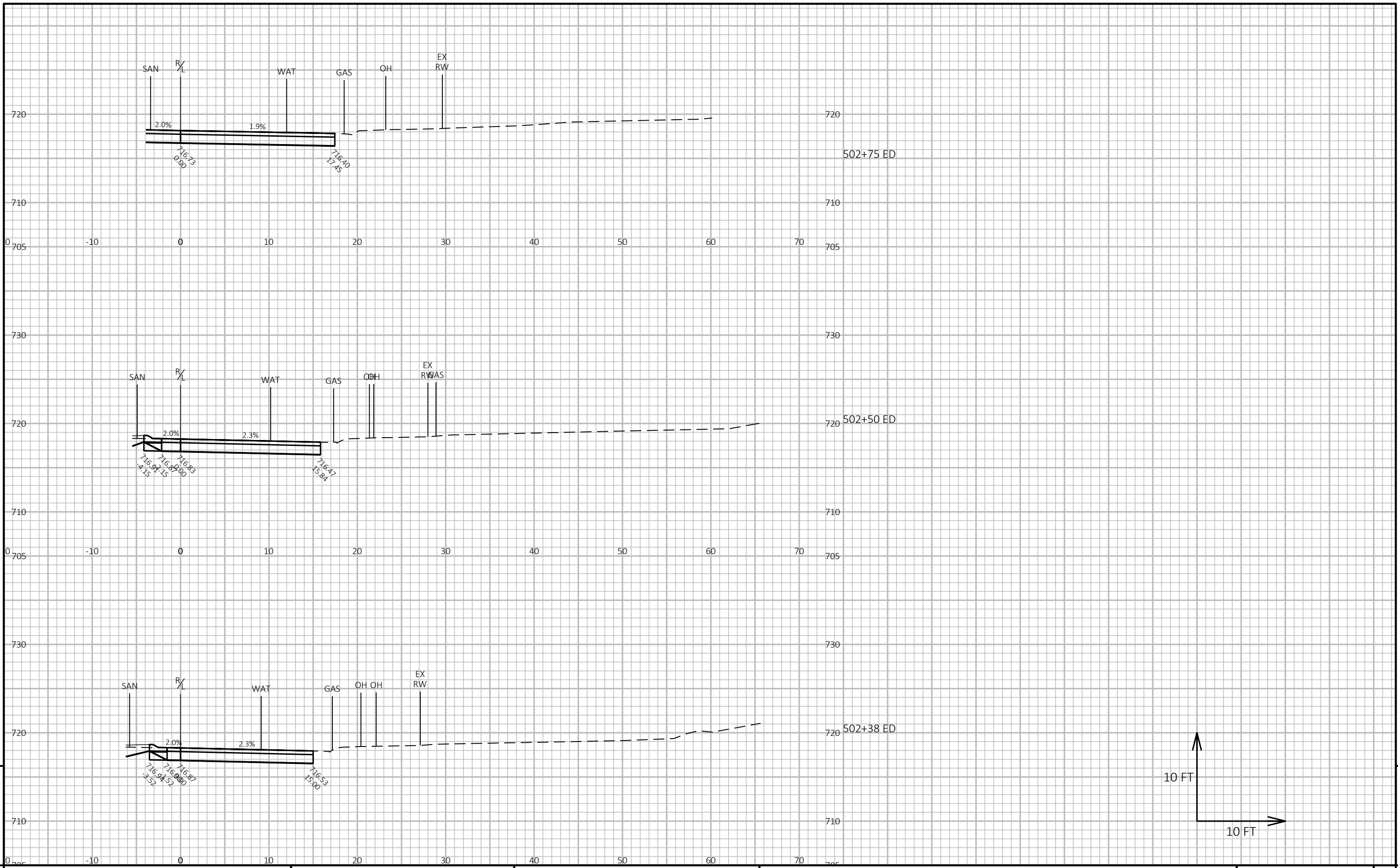
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PROJECT NO: 5991-02-62 HWY: EAST AVENUE N COUNTY: LA CROSSE CROSS SECTIONS: EAST AVENUE N SHEET E

FILE NAME: X:\KO\O\ONALA\160943\5-FINAL-DSGN\51-DRAWINGS\40-TRANSHWY\CIVIL 3D\59910261\SHEETS\PLAN\SEC 09 B CROSS SECTIONS\090201-XS (NORTHBOUND).DWG PLOT DATE: 9/1/2022 9:53 AM PLOT BY: CLARISSA ERBS PLOT NAME: PLOT SCALE: 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49

LAYOUT NAME - 10



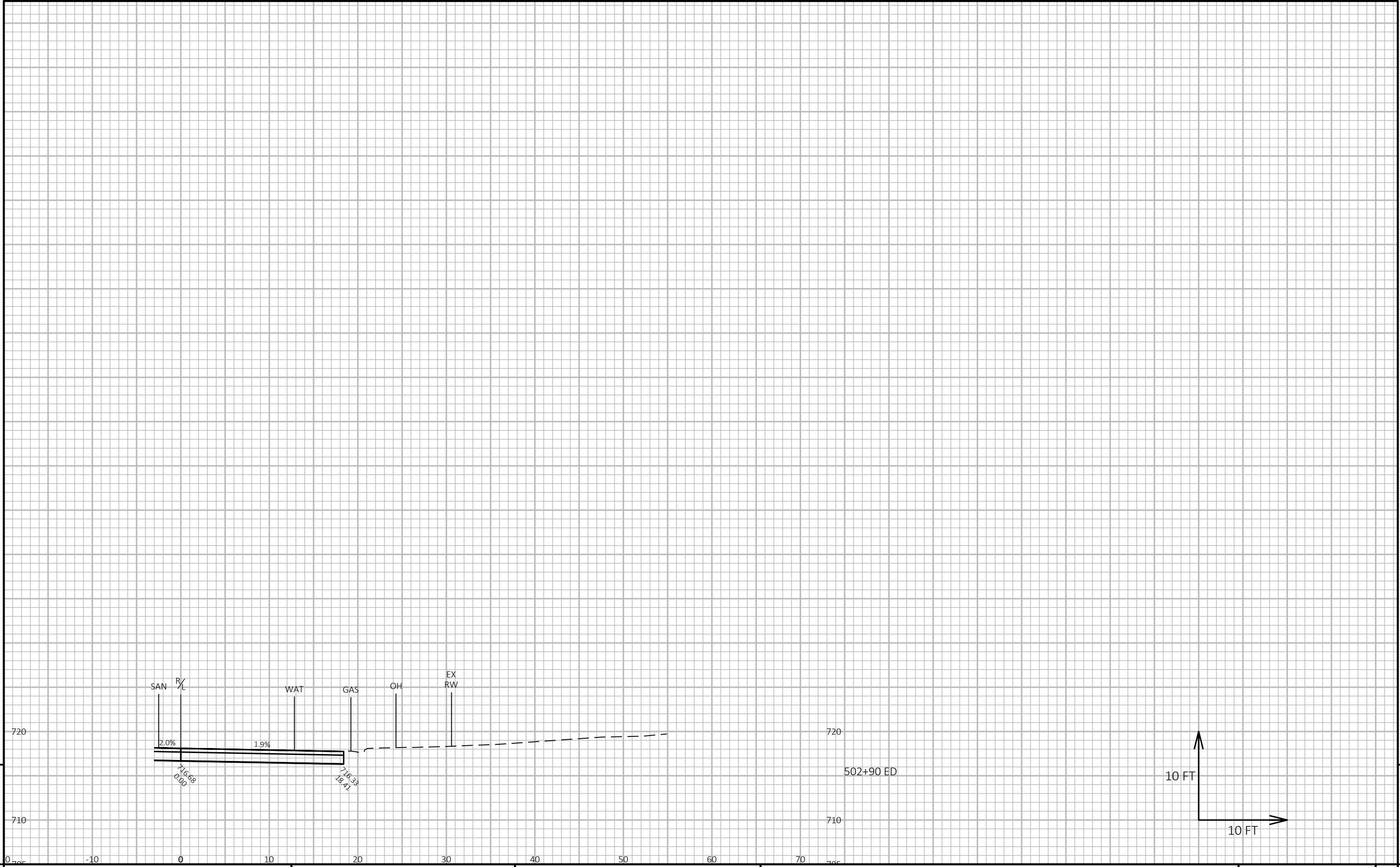
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PROJECT NO: 5991-02-62 HWY: EAST AVENUE N COUNTY: LA CROSSE CROSS SECTIONS: EAST AVENUE N SHEET E

FILE NAME: X:\KO\O\ONALA\160943\5-FINAL-DSGN\51-DRAWINGS\40-TRANSHWY\CIVIL 3D\59910261\SHEETS\PLAN\SEC 09 B CROSS SECTIONS\090201-XS (NORTHBOUND).DWG PLOT DATE: 9/1/2022 9:53 AM PLOT BY: CLARISSA ERBS PLOT NAME: PLOT SCALE: 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49

LAYOUT NAME - 11



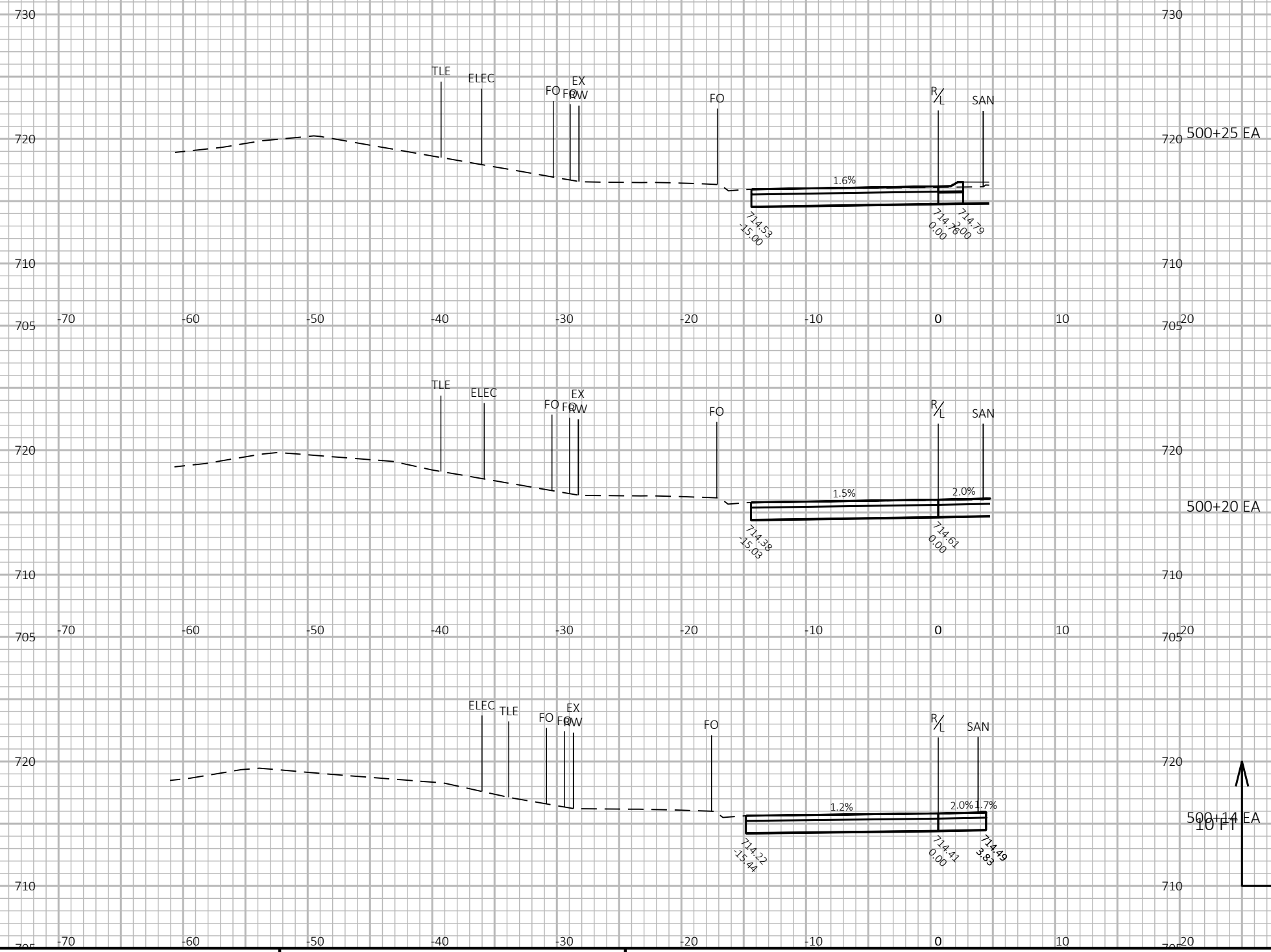
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| | | | | | |
|------------------------|--------------------|-------------------|-------------------------------|-------|---|
| PROJECT NO: 5991-02-62 | HWY: EAST AVENUE N | COUNTY: LA CROSSE | CROSS SECTIONS: EAST AVENUE N | SHEET | E |
|------------------------|--------------------|-------------------|-------------------------------|-------|---|

FILE NAME : X:\KO\O\ONALA\160943\5-FINAL-DSGN\51-DRAWINGS\40-TRANSHWY\CIVIL 3D\59910261\SHEETS\PLAN\SEC 09 B CROSS SECTIONS\090201-XS (NORTHBOUND).DWG PLOT DATE : 9/1/2022 9:53 AM PLOT BY : CLARISSA ERBS PLOT NAME : PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49

LAYOUT NAME - 12



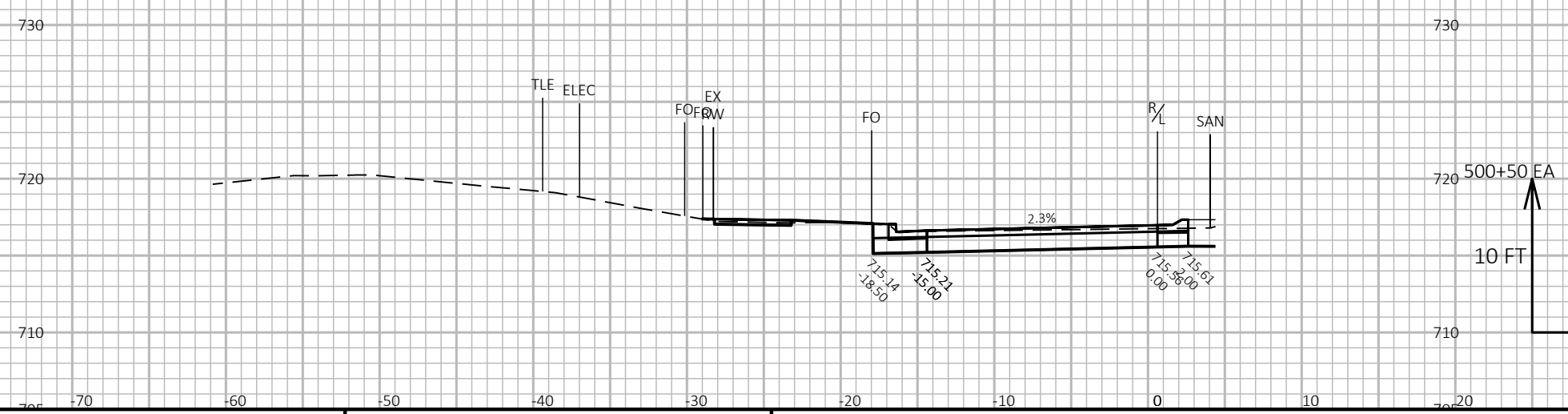
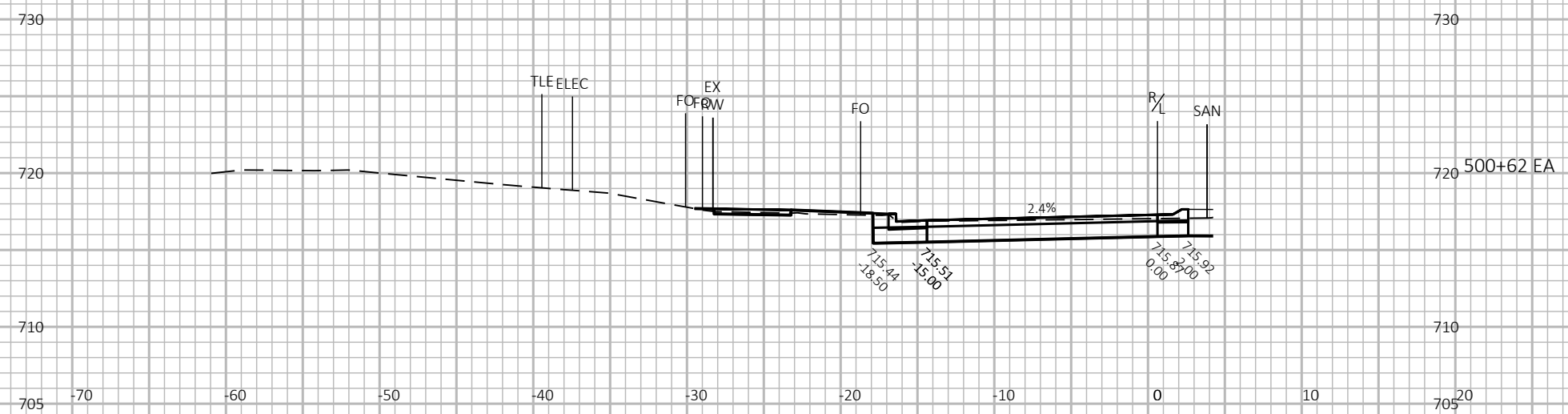
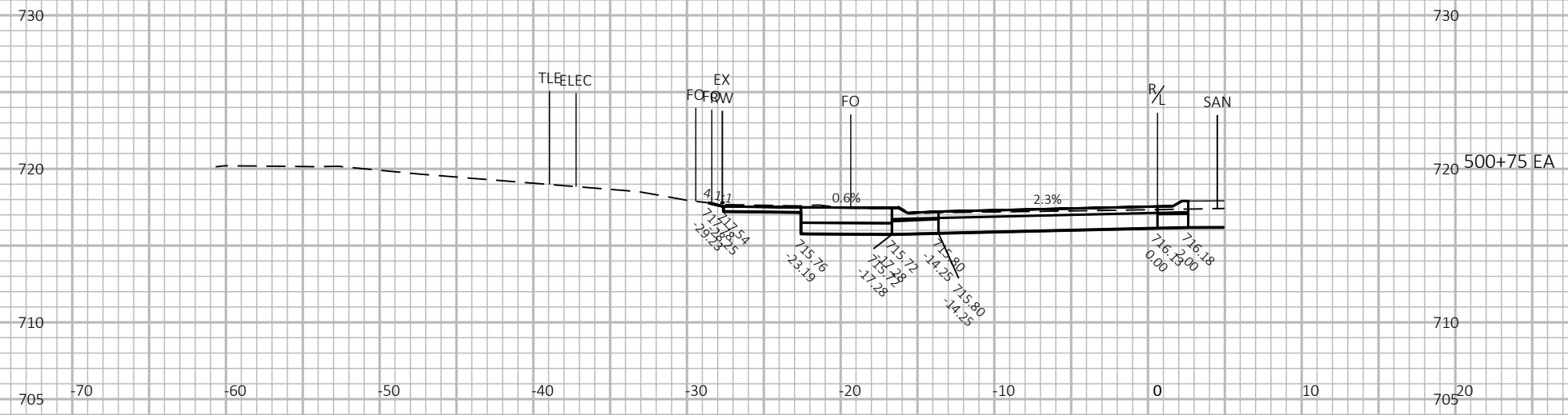
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PROJECT NO: 5991-02-62 HWY: EAST AVENUE N COUNTY: LA CROSSE CROSS SECTIONS: EAST AVENUE N SHEET E

FILE NAME: X:\KO\O\ONALA\160943\5-FINAL-DSGN\51-DRAWINGS\40-TRANSHWY\CIVIL 3D\59910261\SHEETS\PLAN\SEC 09 B CROSS SECTIONS\090201-XS (SOUTHBOUND).DWG PLOT DATE: 9/1/2022 9:54 AM PLOT BY: CLARISSA ERBS PLOT NAME: PLOT SCALE: 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49

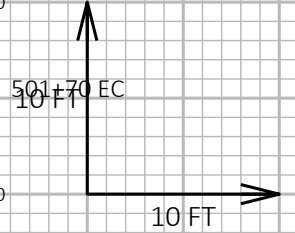
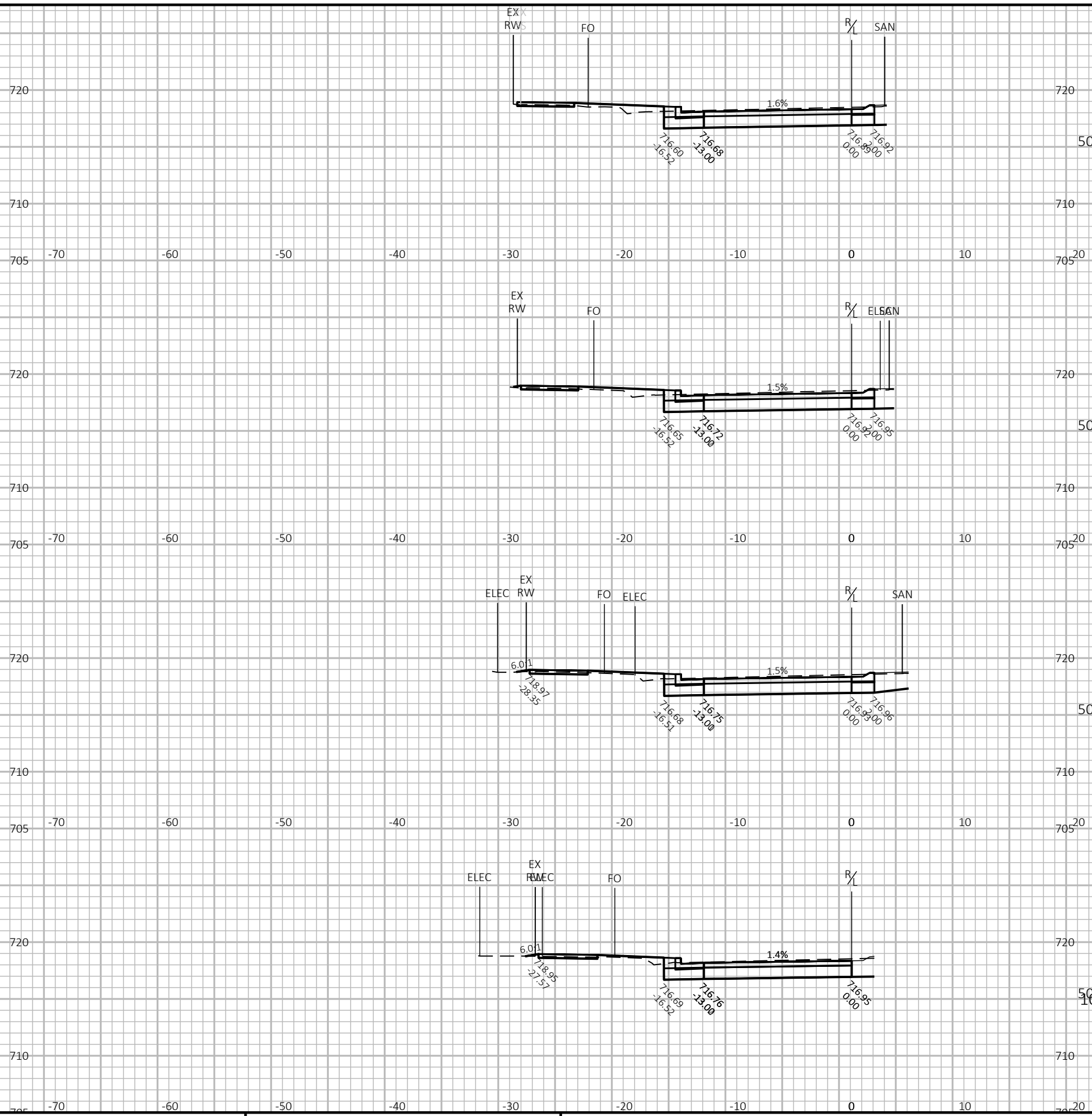
LAYOUT NAME - 01



PROJECT NO: 5991-02-62 HWY: EAST AVENUE N COUNTY: LA CROSSE CROSS SECTIONS: EAST AVENUE N SHEET E

FILE NAME: X:\KO\O\ONALA\160943\5-FINAL-DSGN\51-DRAWINGS\40-TRANSHWY\CIVIL 3D\59910261\SHEETSPLAN\SEC 09 B CROSS SECTIONS\090201-XS (SOUTHBOUND).DWG PLOT DATE: 9/1/2022 9:54 AM PLOT BY: CLARISSA ERBS PLOT NAME: PLOT SCALE: 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49

LAYOUT NAME - 02



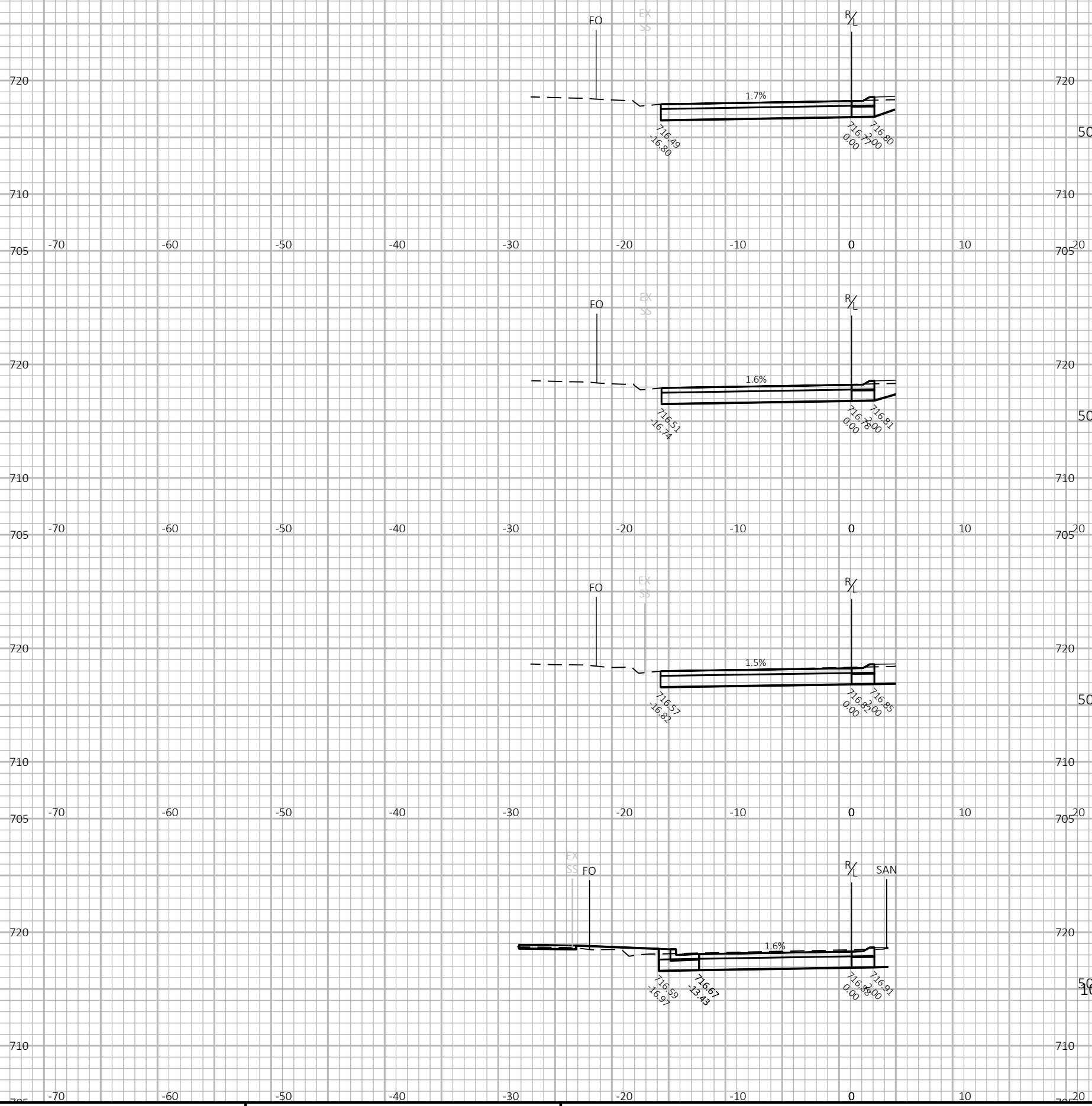
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PROJECT NO: 5991-02-62 HWY: EAST AVENUE N COUNTY: LA CROSSE CROSS SECTIONS: EAST AVENUE N SHEET E

FILE NAME: X:\KO\O\ONALA\160943\5-FINAL-DSGN\51-DRAWINGS\40-TRANSHWY\CIVIL 3D\59910261\SHEETSPLAN\SEC 09 B CROSS SECTIONS\090201-XS (SOUTHBOUND).DWG PLOT DATE: 9/1/2022 9:54 AM PLOT BY: CLARISSA ERBS PLOT NAME: PLOT SCALE: 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49

LAYOUT NAME - 10



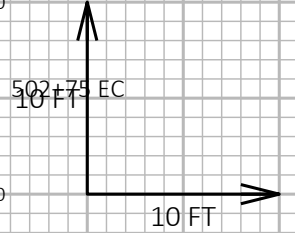
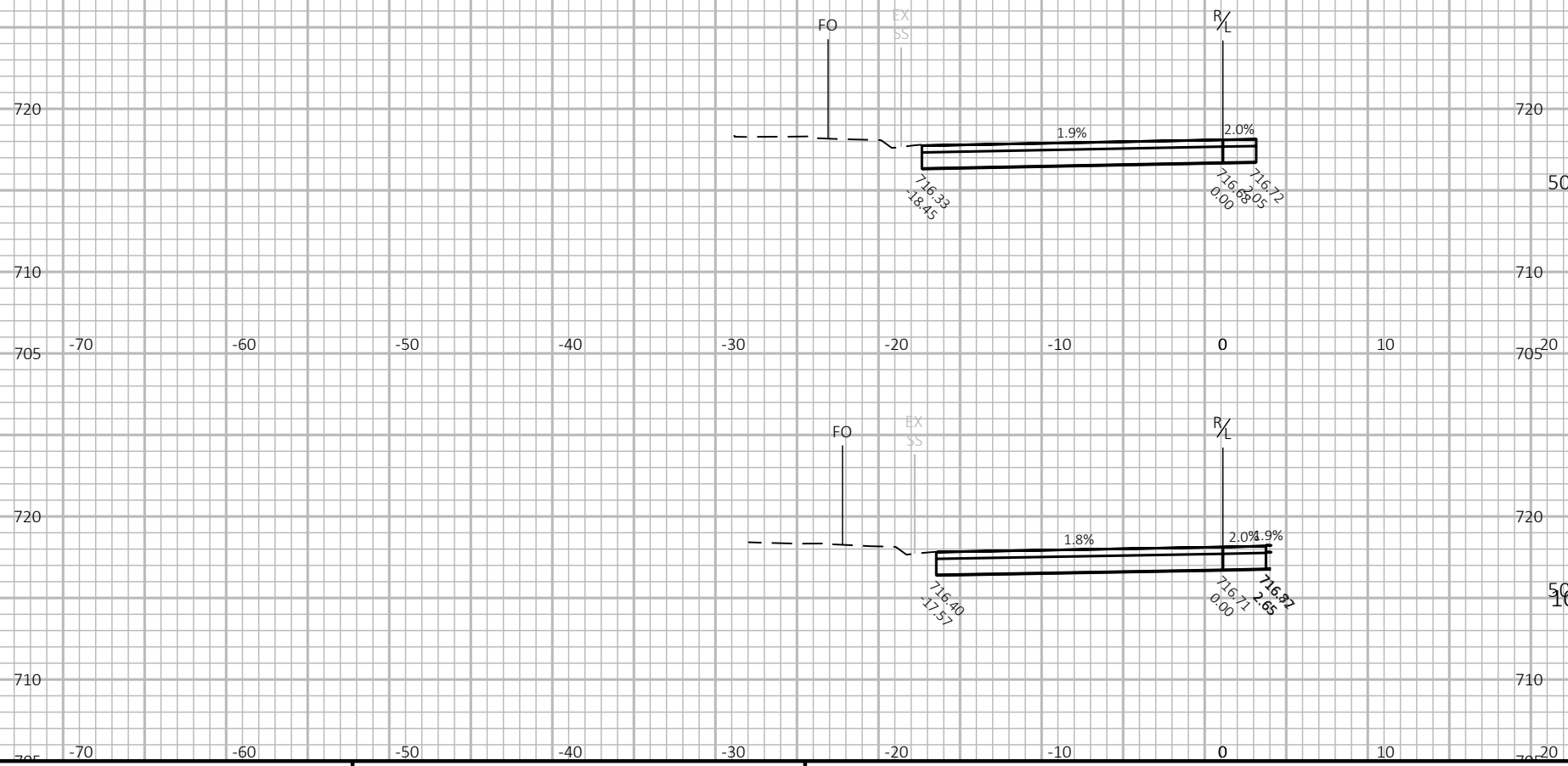
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PROJECT NO: 5991-02-62 HWY: EAST AVENUE N COUNTY: LA CROSSE CROSS SECTIONS: EAST AVENUE N SHEET E

FILE NAME: X:\KO\O\ONALA\160943\5-FINAL-DSGN\51-DRAWINGS\40-TRANSHWY\CIVIL 3D\59910261\SHEETSPLAN\SEC 09 B CROSS SECTIONS\090201-XS (SOUTHBOUND).DWG PLOT DATE: 9/1/2022 9:54 AM PLOT BY: CLARISSA ERBS PLOT NAME: PLOT SCALE: 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49

LAYOUT NAME - 11



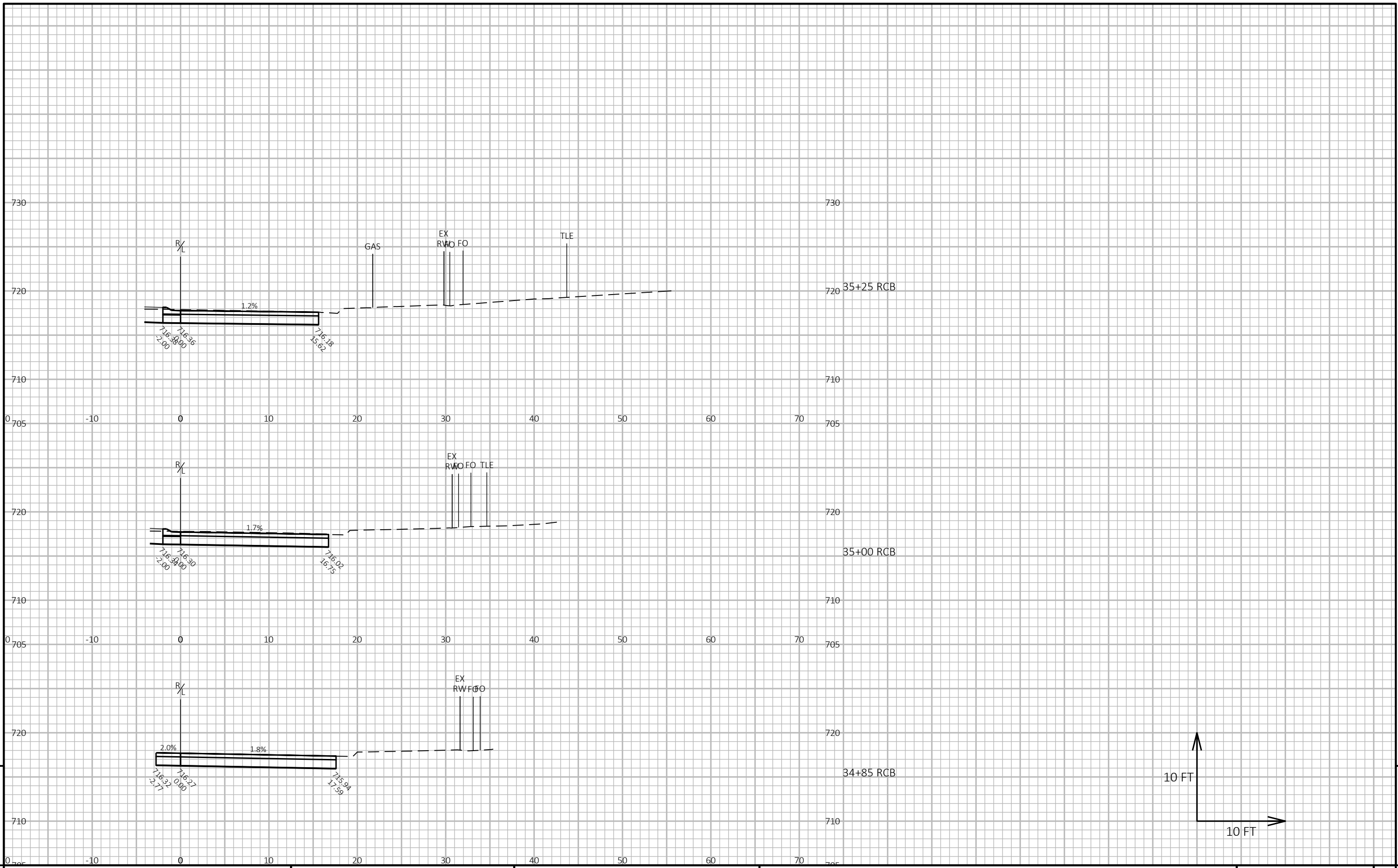
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PROJECT NO: 5991-02-62 HWY: EAST AVENUE N COUNTY: LA CROSSE CROSS SECTIONS: EAST AVENUE N SHEET E

FILE NAME: X:\KO\O\ONALA\160943\5-FINAL-DSGN\51-DRAWINGS\40-TRANSHWY\CIVIL 3D\59910261\SHEETSPLAN\SEC 09 B CROSS SECTIONS\090201-XS (SOUTHBOUND).DWG PLOT DATE: 9/1/2022 9:54 AM PLOT BY: CLARISSA ERBS PLOT NAME: PLOT SCALE: 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49

LAYOUT NAME - 12



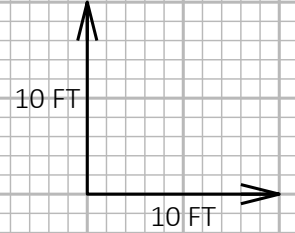
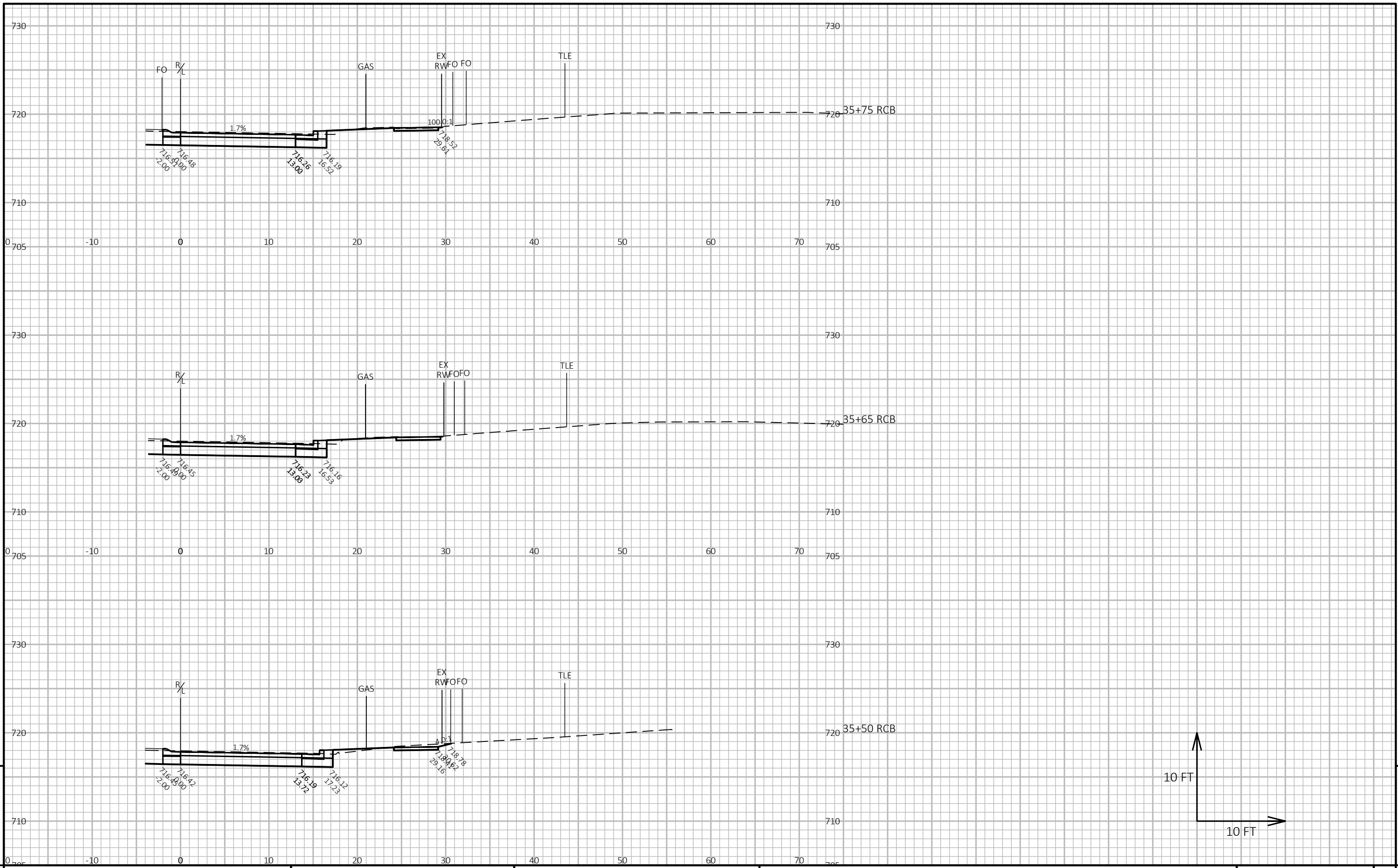
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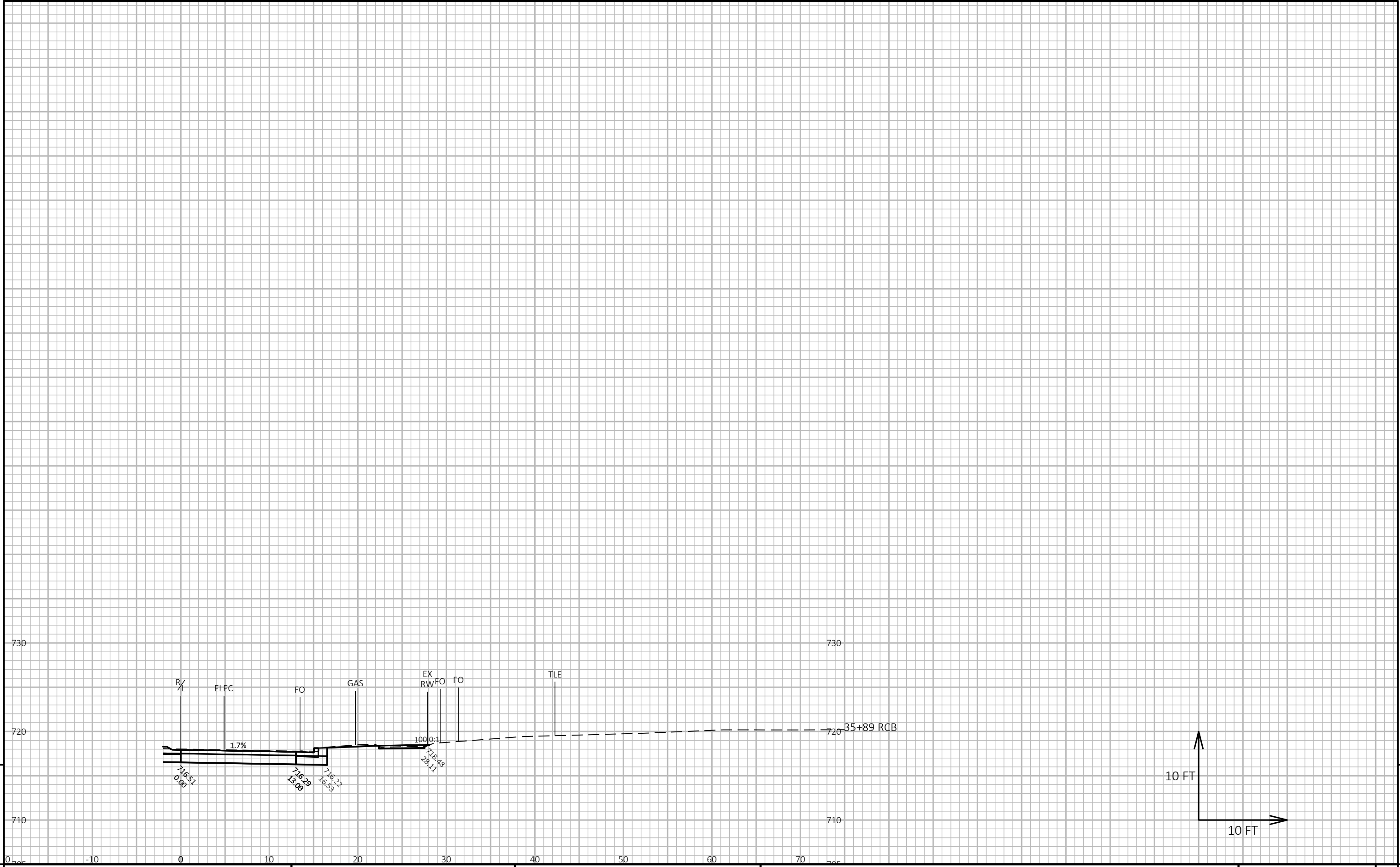
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PROJECT NO: 5991-02-62 HWY: EAST AVENUE N COUNTY: LA CROSSE CROSS SECTIONS: RIDERS CLUB RD SHEET E

FILE NAME : X:\KO\O\ONALA\160943\5-FINAL-DSGN\51-DRAWINGS\40-TRANSHWY\CIVIL 3D\59910261\SHEETS\PLAN\SEC 09 B CROSS SECTIONS\090201-XS (EASTBOUND).DWG PLOT DATE : 9/1/2022 9:54 AM PLOT BY : CLARISSA ERBS PLOT NAME : PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49

LAYOUT NAME - 01





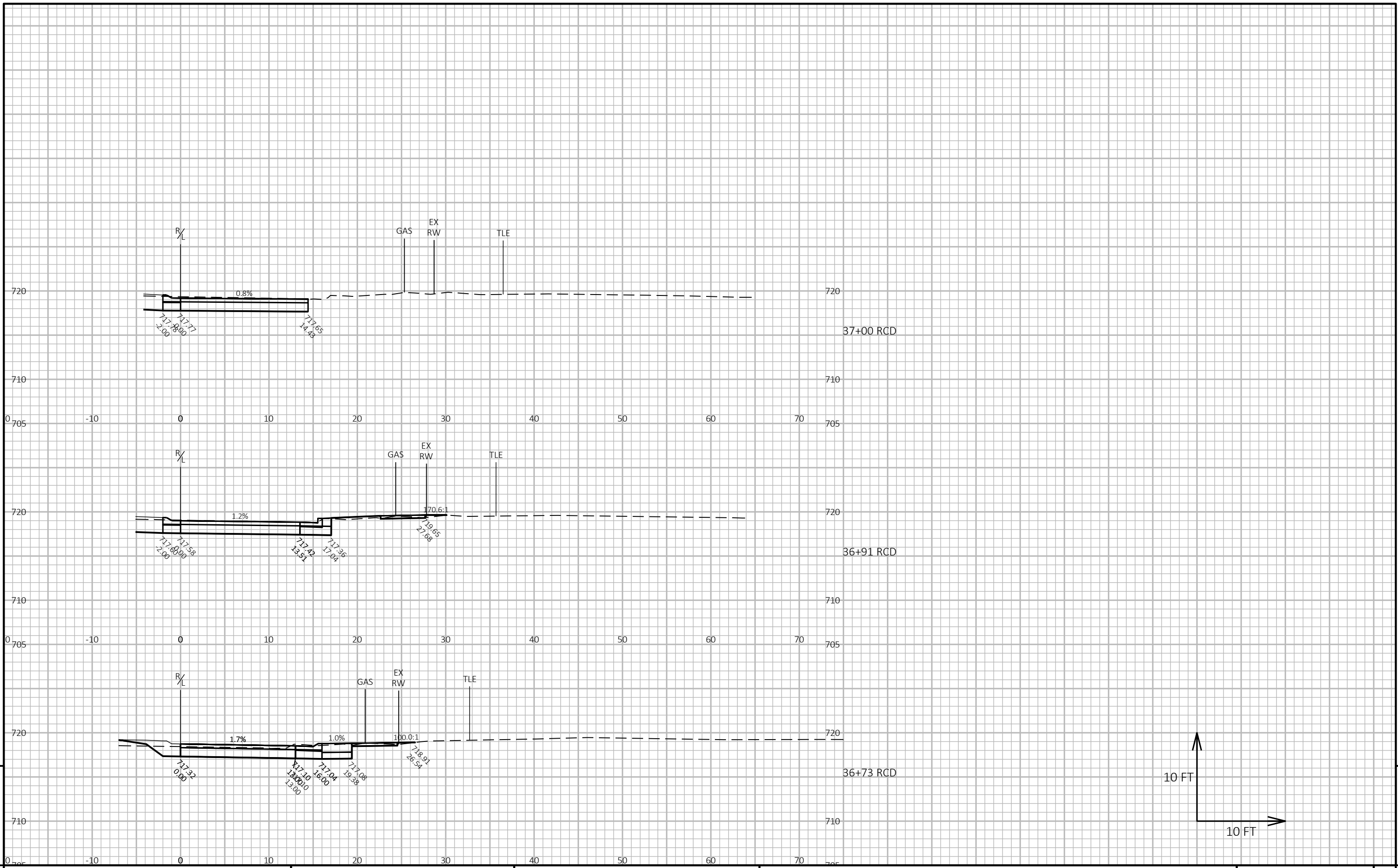
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|------------------------|--------------------|-------------------|--------------------------------|-------|---|
| PROJECT NO: 5991-02-62 | HWY: EAST AVENUE N | COUNTY: LA CROSSE | CROSS SECTIONS: RIDERS CLUB RD | SHEET | E |
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FILE NAME : X:\KO\O\ONALA\160943\5-FINAL-DSGN\51-DRAWINGS\40-TRANSHWY\CIVIL 3D\59910261\SHEETS\PLAN\SEC 09 B CROSS SECTIONS\090201-XS (EASTBOUND).DWG PLOT DATE : 9/1/2022 9:54 AM PLOT BY : CLARISSA ERBS PLOT NAME : PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49

LAYOUT NAME - 03



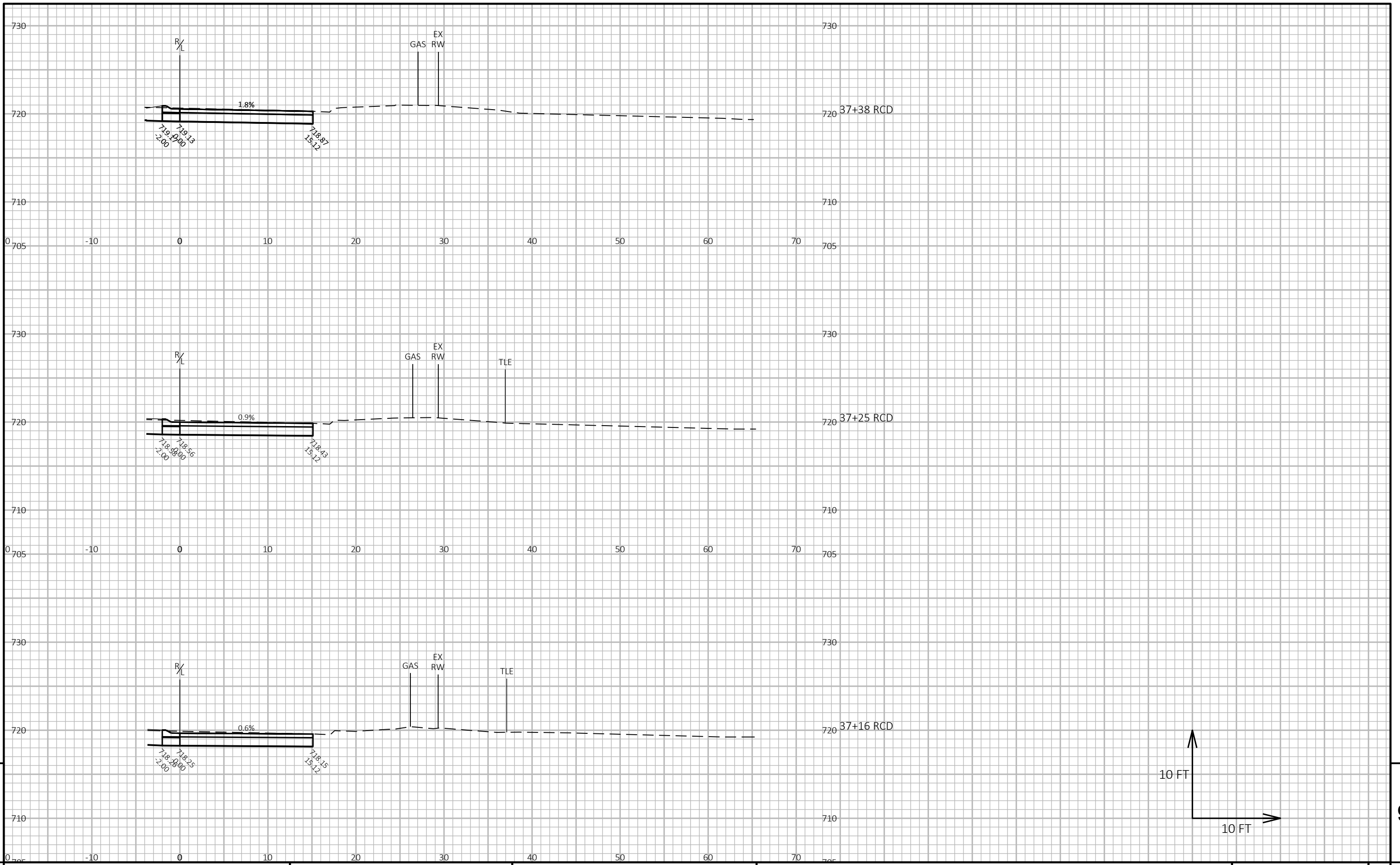
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PROJECT NO: 5991-02-62 HWY: EAST AVENUE N COUNTY: LA CROSSE CROSS SECTIONS: RIDERS CLUB RD SHEET E

FILE NAME : X:\KO\O\ONALA\160943\5-FINAL-DSGN\51-DRAWINGS\40-TRANSHWY\CIVIL 3D\59910261\SHEETS\PLAN\SEC 09 B CROSS SECTIONS\090201-XS (EASTBOUND).DWG PLOT DATE : 9/1/2022 9:54 AM PLOT BY : CLARISSA ERBS PLOT NAME : PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49

LAYOUT NAME - 10



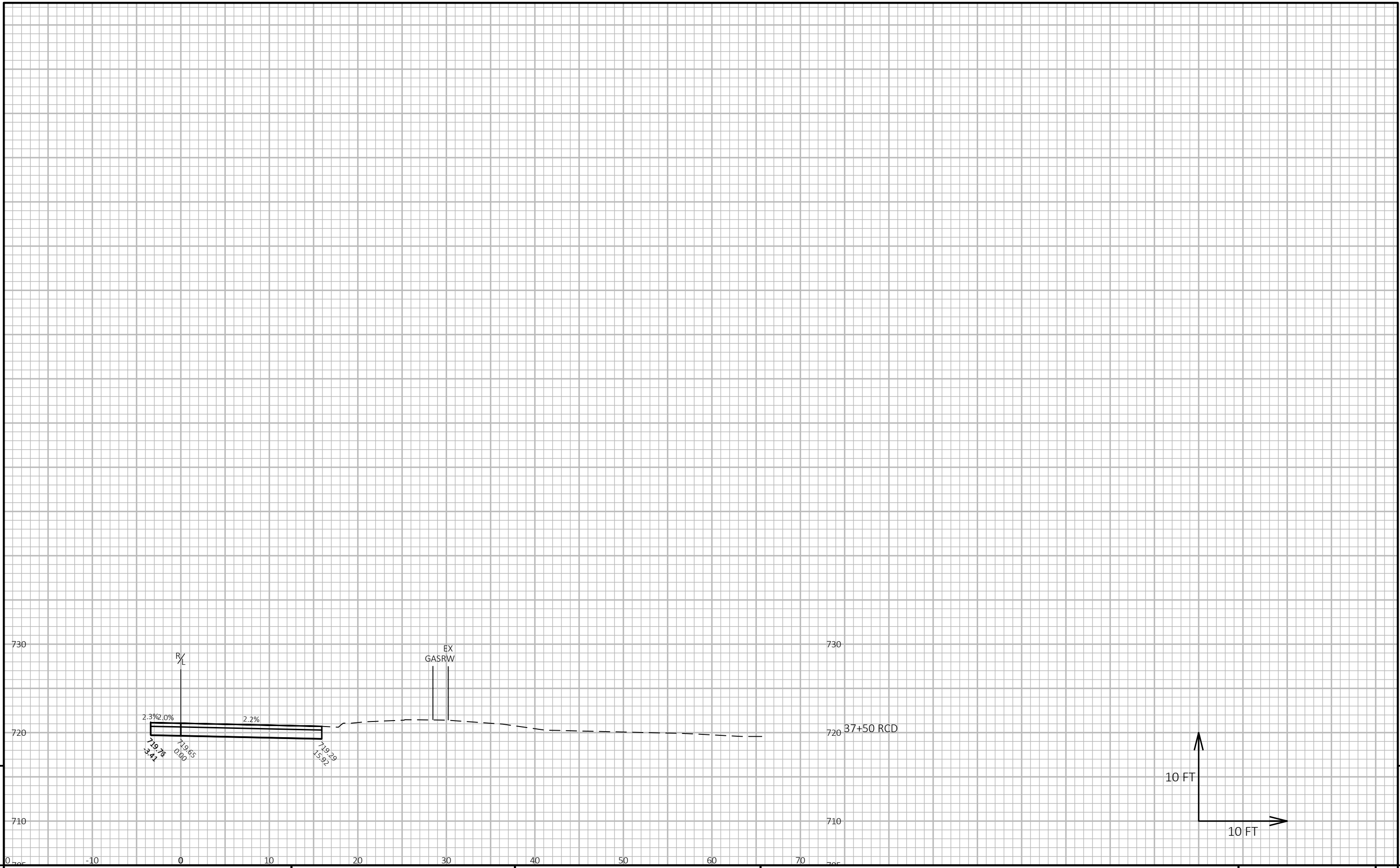
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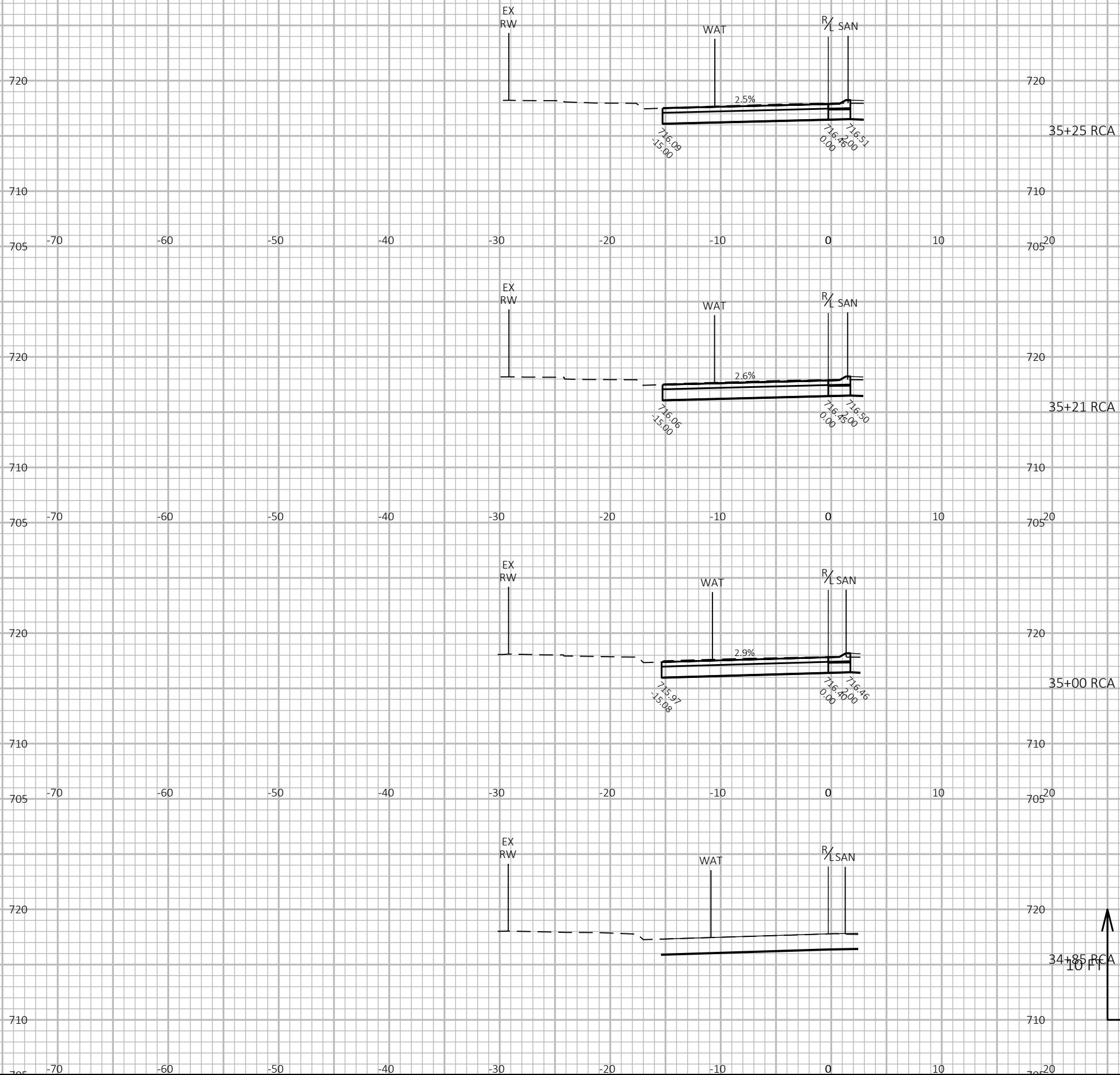
PROJECT NO: 5991-02-62 HWY: EAST AVENUE N COUNTY: LA CROSSE CROSS SECTIONS: RIDERS CLUB RD SHEET E

FILE NAME : X:\KO\O\ONALA\160943\5-FINAL-DSGN\51-DRAWINGS\40-TRANSHWY\CIVIL 3D\59910261\SHEETS\PLAN\SEC 09 B CROSS SECTIONS\090201-XS (EASTBOUND).DWG PLOT DATE : 9/1/2022 9:55 AM PLOT BY : CLARISSA ERBS PLOT NAME : PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49

LAYOUT NAME - 11

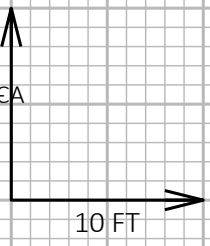
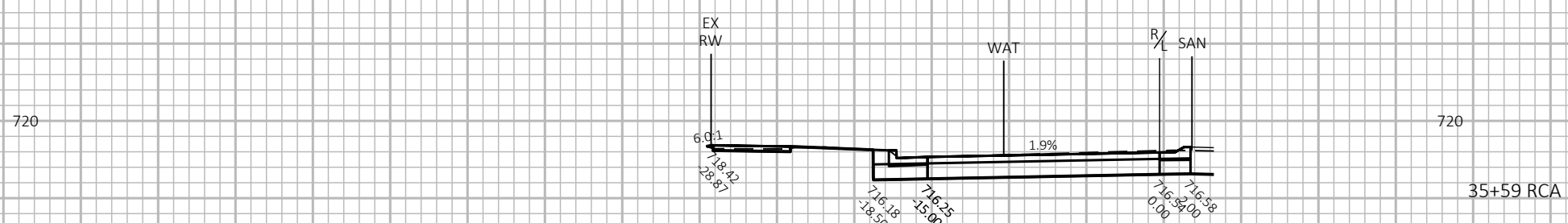
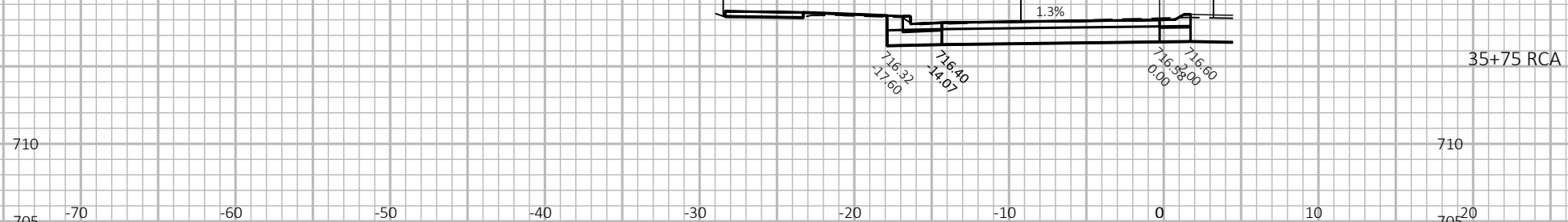
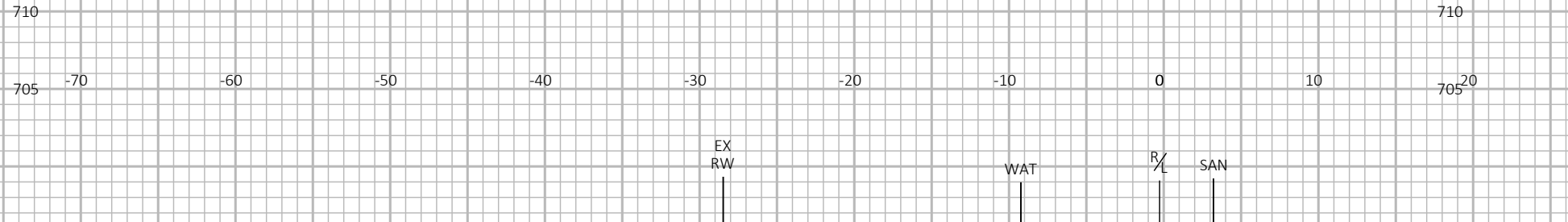
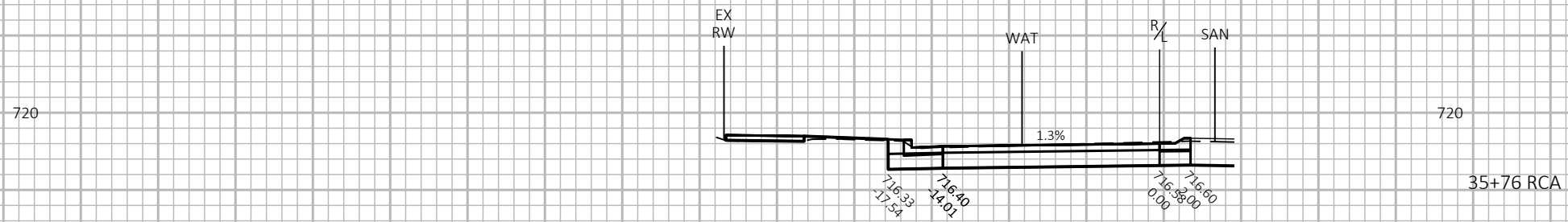


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| PROJECT NO: 5991-02-62 | HWY: EAST AVENUE N | COUNTY: LA CROSSE | CROSS SECTIONS: RIDERS CLUB RD | SHEET | E |
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PROJECT NO: 5991-02-62

HWY: EAST AVENUE N

COUNTY: LA CROSSE

CROSS SECTIONS: RIDERS CLUB RD

SHEET

E

720

710

700

-70

-60

-50

-40

-30

-20

-10

0

10

20

EX
RW

WAT

R/L

ELEC

49.1

718.30

26.57

1.0%

716.49

21.44

0.8%

716.44

16.01

0.0%

716.50

13.00

0.0%

716.62

0.0%

720

710

700

35+86.00

PCA

10 FT

10 FT

9

9

PROJECT NO: 5991-02-62

HWY: EAST AVENUE N

COUNTY: LA CROSSE

CROSS SECTIONS: RIDERS CLUB RD

SHEET

E

FILE NAME : X:\KO\O\ONALA\160943\5-FINAL-DSGN\51-DRAWINGS\40-TRANSHWY\CIVIL 3D\59910261\SHEETS\PLAN\SEC 09 B CROSS SECTIONS\090201-XS (WESTBOUND).DWG
LAYOUT NAME - 03

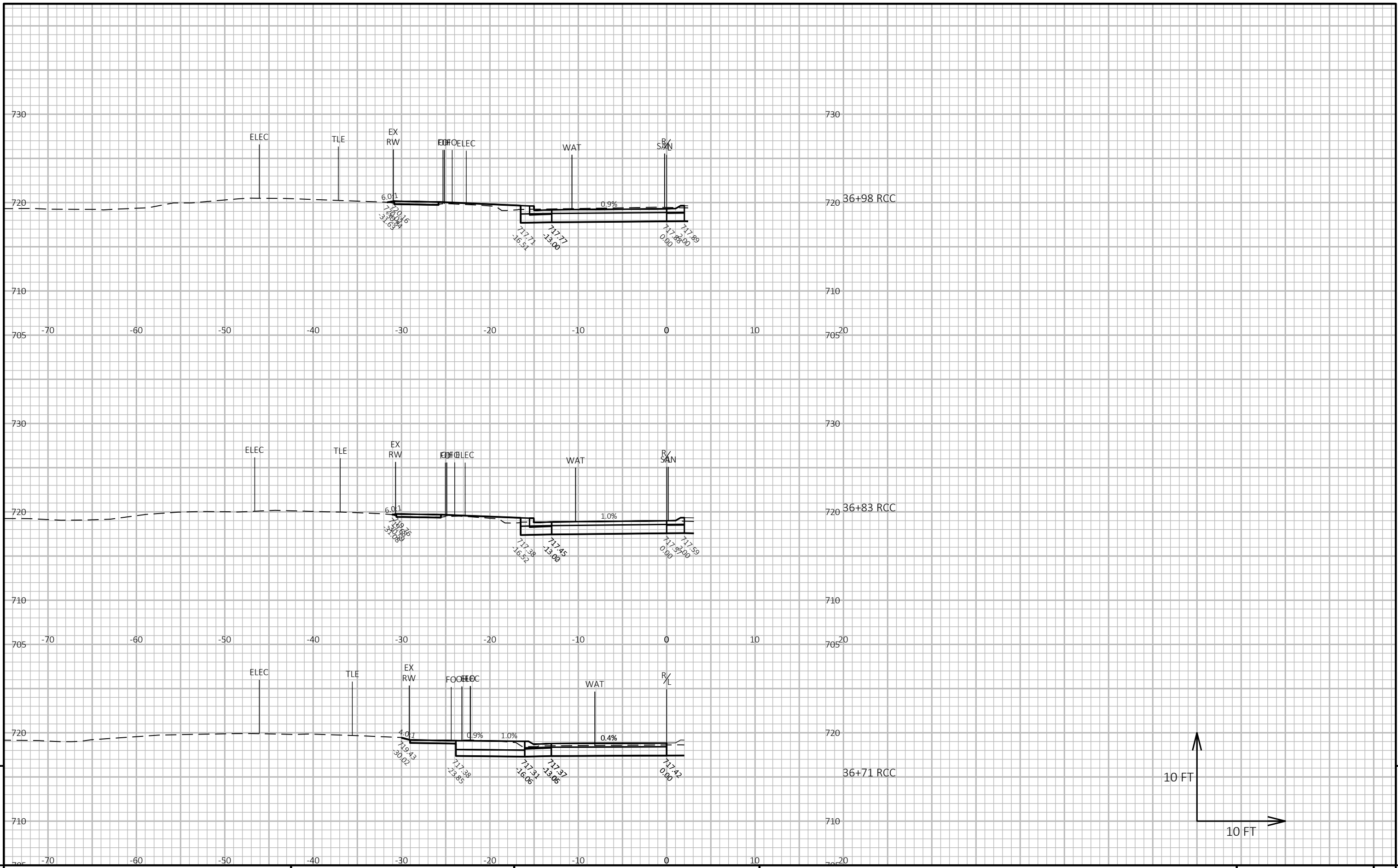
PLOT DATE : 9/1/2022 9:55 AM

PLOT BY : CLARISSA ERBS

PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADD SHEET 49



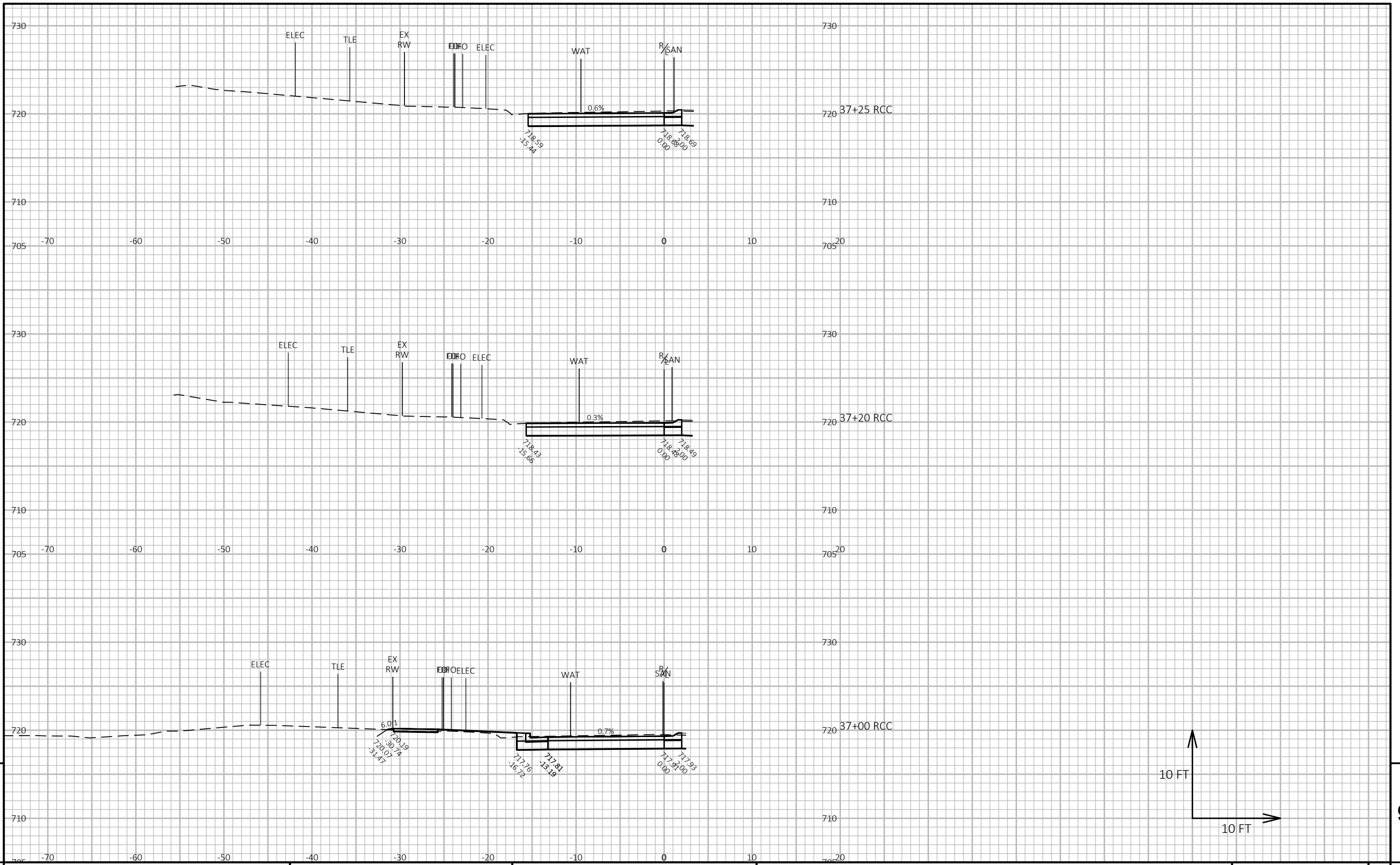
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PROJECT NO: 5991-02-62 HWY: EAST AVENUE N COUNTY: LA CROSSE CROSS SECTIONS: RIDERS CLUB RD SHEET E

FILE NAME: X:\KO\O\ONALA\160943\5-FINAL-DSGN\51-DRAWINGS\40-TRANSHWY\CIVIL 3D\59910261\SHEETS\PLAN\SEC 09 B CROSS SECTIONS\090201-XS (WESTBOUND).DWG PLOT DATE: 9/1/2022 9:55 AM PLOT BY: CLARISSA ERBS PLOT NAME: PLOT SCALE: 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49

LAYOUT NAME - 10



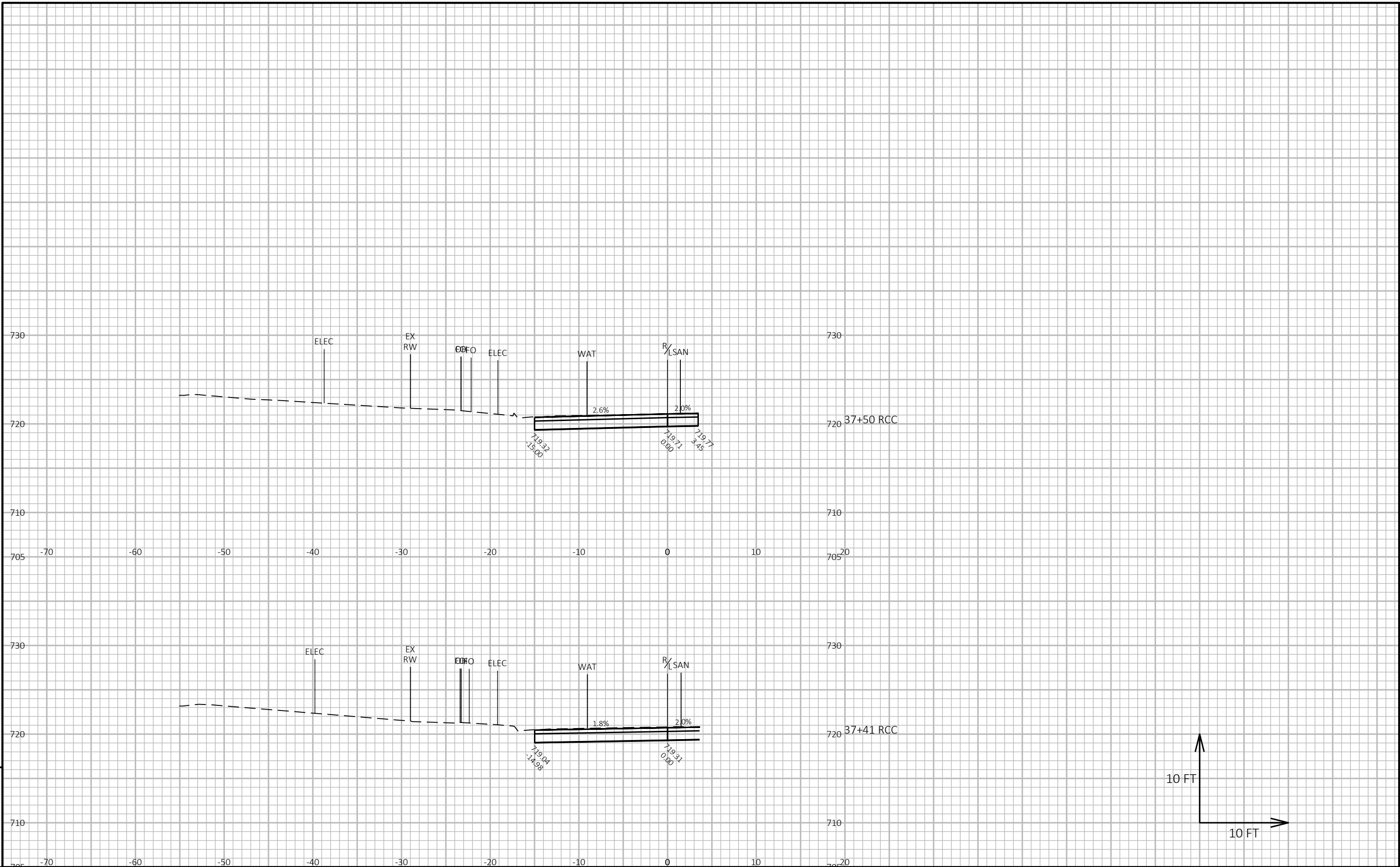
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PROJECT NO: 5991-02-62 HWY: EAST AVENUE N COUNTY: LA CROSSE CROSS SECTIONS: RIDERS CLUB RD SHEET E

FILE NAME: X:\KO\O\ONALA\160943\5-FINAL-DSGN\51-DRAWINGS\40-TRANSHWY\CIVIL 3D\59910261\SHEETS\PLAN\SEC 09 B CROSS SECTIONS\090201-XS (WESTBOUND).DWG PLOT DATE: 9/1/2022 9:55 AM PLOT BY: CLARISSA ERBS PLOT NAME: PLOT SCALE: 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49

LAYOUT NAME - 11



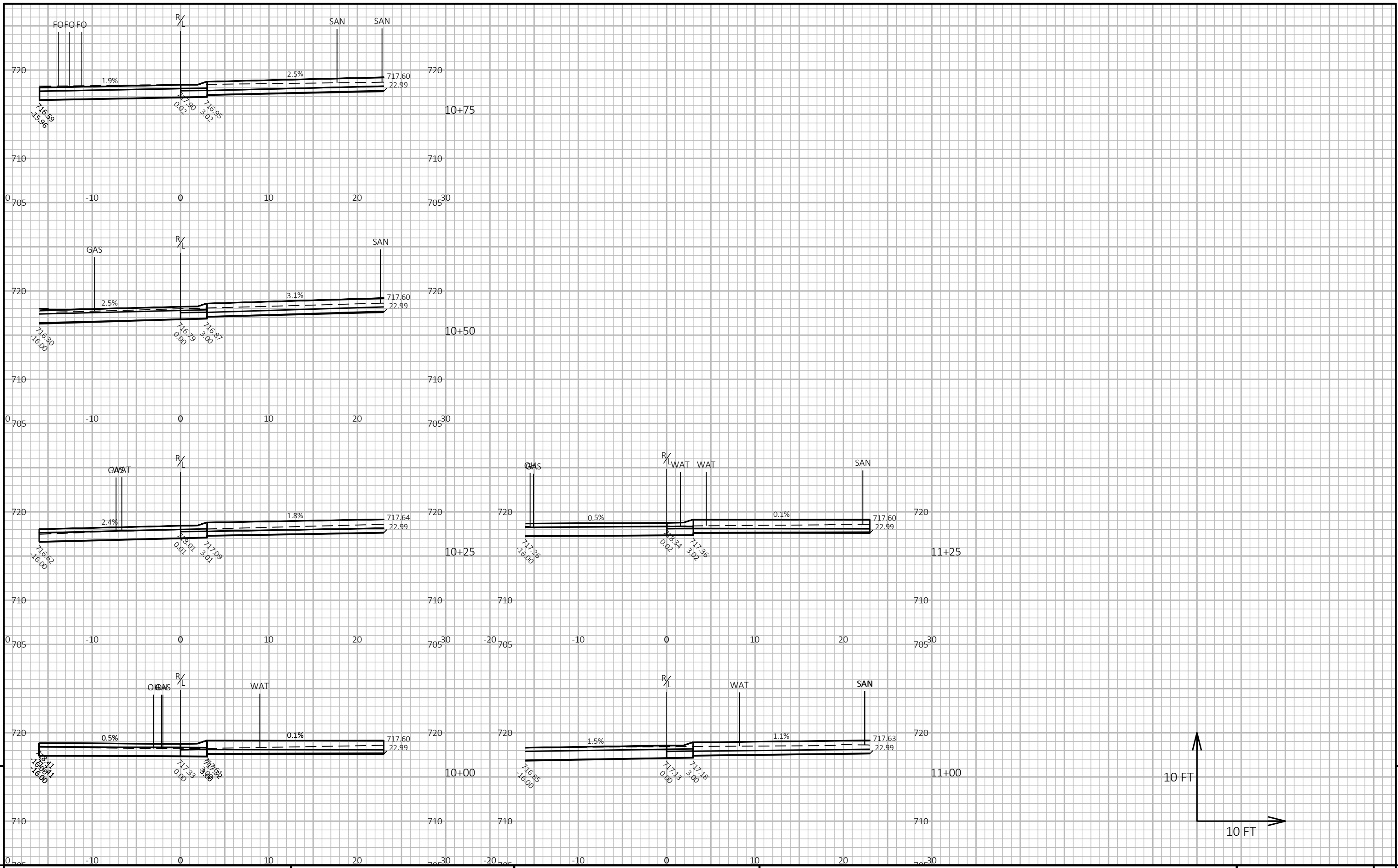
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| PROJECT NO: 5991-02-62 | HWY: EAST AVENUE N | COUNTY: LA CROSSE | CROSS SECTIONS: RIDERS CLUB RD | SHEET | E |
|------------------------|--------------------|-------------------|--------------------------------|-------|---|

FILE NAME : X:\KO\O\ONALA\160943\5-FINAL-DSGN\51-DRAWINGS\40-TRANSHWY\CIVIL 3D\59910261\SHEETS\PLAN\SEC 09 B CROSS SECTIONS\090201-XS (WESTBOUND).DWG PLOT DATE : 9/1/2022 9:55 AM PLOT BY : CLARISSA ERBS PLOT NAME : PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49

LAYOUT NAME - 12



PROJECT NO: 5991-02-62 HWY: EAST AVENUE N COUNTY: LA CROSSE CROSS SECTIONS: CENTER ISLAND SHEET E

Notes



Wisconsin Department of Transportation

Dedicated people creating transportation solutions through innovation and exceptional service.

<http://www.dot.wisconsin.gov>