

ORDER OF SHEETS

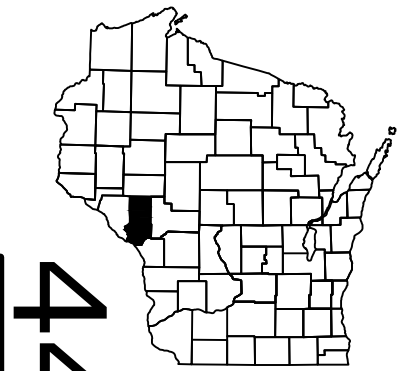
Section No.	1	Title
Section No.	2	Typical Sections and Details
Section No.	3	Estimate of Quantities
Section No.	3	Miscellaneous Quantities
Section No.	4	Right of Way Plat
Section No.	5	Plan and Profile
Section No.	6	Standard Detail Drawings
Section No.	7	Sign Plates
Section No.	8	Structure Plans
Section No.	9	Computer Earthwork Data
Section No.	9	Cross Sections

TOTAL SHEETS = 174

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION  
PLAN OF PROPOSED IMPROVEMENT  
**LA CROSSE - TREMPEALEAU**  
LA CROSSE/TREMPEALEAU COUNTY LINE TO 10TH ST  
**STH 35**  
TREMPEALEAU COUNTY

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
7140-00-70		

STATE PROJECT NUMBER  
**7140-00-70**



44



END PROJECT 7140-00-70  
STA 426+08.17

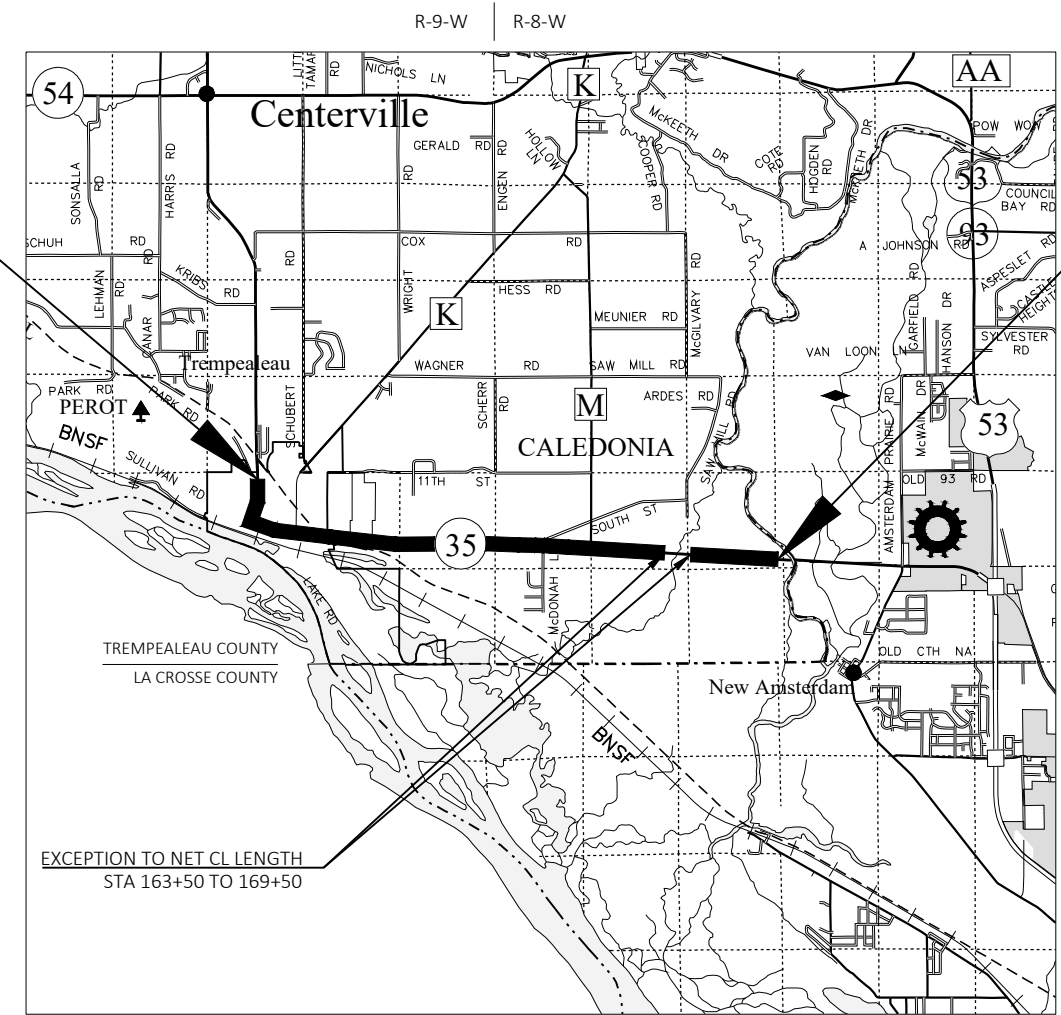
BEGIN PROJECT 7140-00-70  
STA 107+00  
Y = 305,802.317  
X = 852,556.083

DESIGN DESIGNATION STH 35

A.A.D.T.	2024	=	5400
A.A.D.T.	2044	=	6950
D.H.V.		=	626
D.D.		=	50/50
T.		=	10.0%
DESIGN SPEED		=	35/60 MPH
ESALS		=	1,200,000

CONVENTIONAL SYMBOLS

PLAN		PROFILE	
CORPORATE LIMITS		GRADE LINE	
PROPERTY LINE		ORIGINAL GROUND	
LOT LINE		MARSH OR ROCK PROFILE (To be noted as such)	
LIMITED HIGHWAY EASEMENT		SPECIAL DITCH	
EXISTING RIGHT OF WAY		GRADE ELEVATION	
PROPOSED OR NEW R/W LINE		CULVERT (Profile View)	
SLOPE INTERCEPT		UTILITIES	
REFERENCE LINE		ELECTRIC	
EXISTING CULVERT		FIBER OPTIC	
PROPOSED CULVERT (Box or Pipe)		GAS	
COMBUSTIBLE FLUIDS		SANITARY SEWER	
MARSH AREA		STORM SEWER	
WOODED OR SHRUB AREA		TELEPHONE	
		WATER	
		UTILITY PEDESTAL	
		POWER POLE	
		TELEPHONE POLE	



LAYOUT  
SCALE 0 2.0 MI  
TOTAL NET LENGTH OF CENTERLINE = 5.930 MI.

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS), TREMPEALEAU NAD83 (2011), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES. ELEVATIONS ARE REFERENCED TO NAVD 88 (2012). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 12A.

ORIGINAL PLANS PREPARED BY

DATE: 10/25/22  
*Tammy Tucker*  
(Professional Engineer Signature)

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

PREPARED BY	Surveyor	CORRE INC/CBS SQUARED, INC.
Designer		CBS SQUARED, INC.
Project Manager		KYLE MCLEAN
Regional Examiner		JENNIFER OLDENBURG
Regional Supervisor		DAVID KOEPP

APPROVED FOR THE DEPARTMENT

DATE: 10/27/2022  
*Kyle McLean*  
(Signature)

GENERAL NOTES

NO TREES OR SHRUBS ARE TO BE REMOVED WITHOUT APPROVAL OF THE ENGINEER.

THE LOCATIONS OF THE EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

WHEN THE QUANTITY OF BASE AGGREGATE IS MEASURED BY THE TON OR CUBIC YARD, THE DEPTH OR THICKNESS OF THE LAYER SHOWN ON THE PLANS IS APPROXIMATE AND THE ACTUAL THICKNESS WILL DEPEND ON THE DISTRIBUTION OF THE MATERIAL AS DIRECTED BY THE ENGINEER.

DISTURBED AREAS WITHIN THE RIGHT OF WAY, EXCEPT THE AREAS WITHIN THE FINISHED SHOULDER POINTS, SHALL BE FERTILIZED, SEEDED AND EMATTED AS SPECIFIED IN PLANS.

WHEN PORTIONS OF EXISTING ASPHALTIC SURFACES ARE TO BE REMOVED TO ACCOMMODATE NEW CONSTRUCTION, THE LINE OF SUCH REMOVAL SHALL BE NEATLY DELINEATED WITH A SAW CUT JOINT THROUGH THE ASPHALTIC SURFACE SO THAT REMOVAL OF THE ASPHALT SHALL BE ACCOMPLISHED WITHOUT DAMAGE TO REMAINING PORTIONS. THE LOCATION OF SAW JOINTS AND THE AMOUNT REMOVED AT SIDE ROADS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

CROSS SLOPES AS SHOWN ON THE TYPICAL SECTION WILL VARY AT THE INTERSECTIONS.

APPLY TACK COAT AT A RATE OF 0.07 GAL/SY TO MILLED PAVEMENT SURFACES AND 0.05 GAL/SY BETWEEN LAYERS OF HMA PAVEMENT.

HMA PAVEMENT WEIGHT CALCULATIONS ARE BASED ON 112 LB/SY/IN.

THE CONTRACTOR'S PAVING OPERATIONS SHALL BE CONSISTENT WITH THE PLAN TYPICAL SECTIONS AND CONSTRUCTED TO PREVENT HMA LONGITUDINAL JOINTS FROM BEING LOCATED WITHIN A DRIVING, TURNING, PASSING OR PARKING LANES.

STANDARD ABBREVIATIONS

Table with 2 columns: Abbreviation and Full Name. Includes AC (ACRE), AGG (AGGREGATE), AH (AHEAD), AADT (ANNUAL AVERAGE DAILY TRAFFIC), ASPH (ASPHALTIC), etc.

UTILITY CONTACTS

ELECTRIC

DAIRYLAND POWER COOP - ELECTRICITY
MICHAEL LYDON
3200 EAST AVE S
P.O. BOX 817
LA CROSSE, WI 54602-0817
(608) 787-1381
MICHAEL.LYDON@DAIRYLANDPOWER.COM

TREMPEALEAU MUNICIPAL UTILITIES - ELECTRIC
KIM GANZ
24455 3RD ST
P.O. BOX 247
TREMPEALEAU, WI 54661
(608) 534-6434
(608) 386-3731 MOBILE
KGANZ@TREMPEALEAUWI.COM

GAS/PETROLEUM

MIDWEST NATURAL GAS, INC - GAS/PETROLEUM
RICK SCHERMERHORN
3600 STATE HIGHWAY 157
SUITE 200
LA CROSSE, WI 54602-0429
(608) 781-1011
(608) 780-0210 MOBILE
RICKS@MIDWESTNATURALGAS.COM

SEWER

TREMPEALEAU MUNICIPAL UTILITIES - SEWER
TODD LAKEY
24455 3RD ST
P.O. BOX 247
TREMPEALEAU, WI 54661
(608) 792-1735
TLAKEY@TREMPEALEAUWI.COM

COMMUNICATIONS

TRI-COUNTY COM COOP - COMMUNICATION LINE
BUCK WEBB
417 5TH AVE N
P.O. BOX 578
STRUM, WI 54770
(715) 695-2691
BWEBB@TCCPRO.NET

CENTURYLINK/BRIGHTSPEED - COMMUNICATION LINE
TOM MURRAY
333 NORTH FRONT STREET
LA CROSSE, WI 54601
(608) 615-4169
(608) 780-0895 - MOBILE
TOM.L.MURRAY@BRIGHTSPEED.COM

WINDSTREAM - COMMUNICATION LINE
LORI KETTER
314 N DANZ AVE
GREEN BAY, WI 54302
(414) 274-9215
(920) 410-6902 MOBILE
LORI.KETTER@WINDSTREAM.COM

WATER

TREMPEALEAU MUNICIPAL UTILITIES - WATER
TODD LAKEY
24455 3RD ST
P.O. BOX 247
TREMPEALEAU, WI 54661
(608) 792-1735
TLAKEY@TREMPEALEAUWI.COM

RUNOFF COEFFICIENT TABLE

Table with columns for Hydrologic Soil Group (A, B, C, D) and Slope Range (Percent). Rows include Land Use (ROW CROPS, MEDIAN STRIP-TURF, SIDE SLOPE-TURF) and Pavement types (ASPHALT, CONCRETE, BRICK, etc.).

TOTAL PROJECT AREA = 72.50 ACRES
TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = 1.160 ACRES

AREA CONTACTS

WISDOT CONTACT

KYLE MCLEAN
WISDOT NORTHWEST REGION
718 W CLAIREMONT AVENUE
EAU CLAIRE, WI 53701
(715) 225-9442
KYLE.MCLEAN@DOT.WI.GOV

DESIGN CONTACT

TAMMY TUCKER
CBS SQUARED, INC.
2500 E. ENTERPRISE AVE, SUITE A
APPLETON, WI 54913
(262) 613-8999
TTUCKER@CBSQUAREDINC.COM

WISCONSIN DNR CONTACT

AMY LESIK
DNR NORTHWEST REGION
1300 WEST CLAIREMONT AVE
EAU CLAIRE, WI 54701
(715) 495-1903
AMYL.LESIK@WISCONSIN.GOV

ORDER OF DETAIL SHEETS

- PROJECT OVERVIEW
TYPICAL SECTIONS
CONSTRUCTION DETAILS
INTERSECTION DETAILS
CURB RAMP DETAILS
UTILITY PLANS
LIGHTING PLAN
PAVEMENT MARKING PLAN
TRAFFIC CONTROL

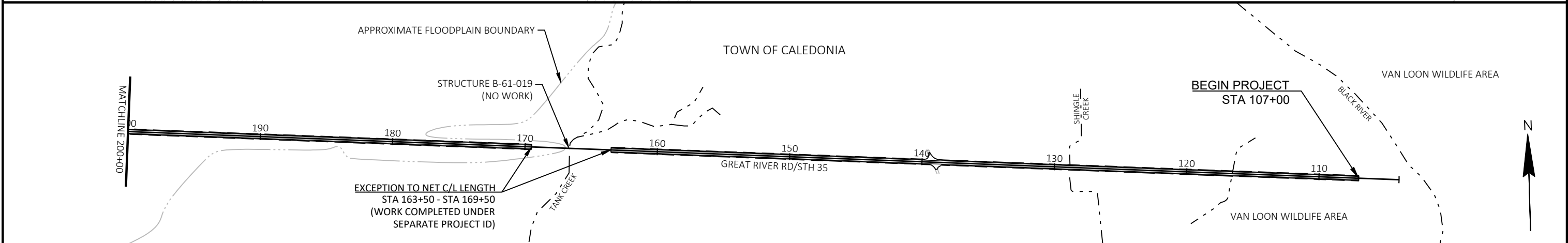
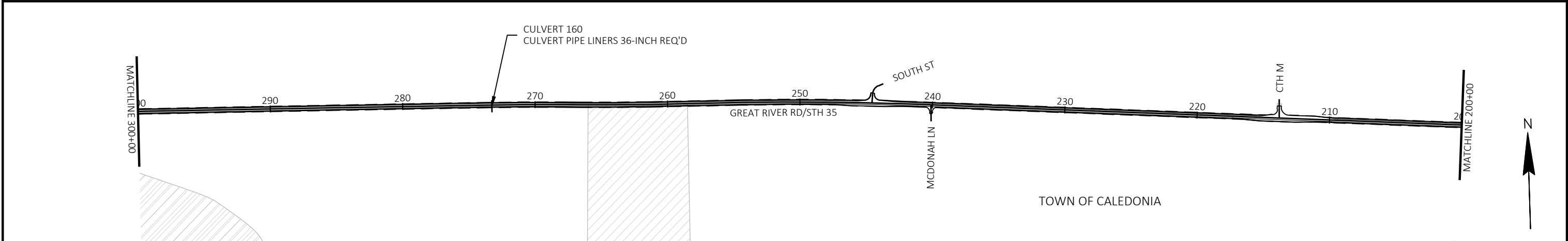
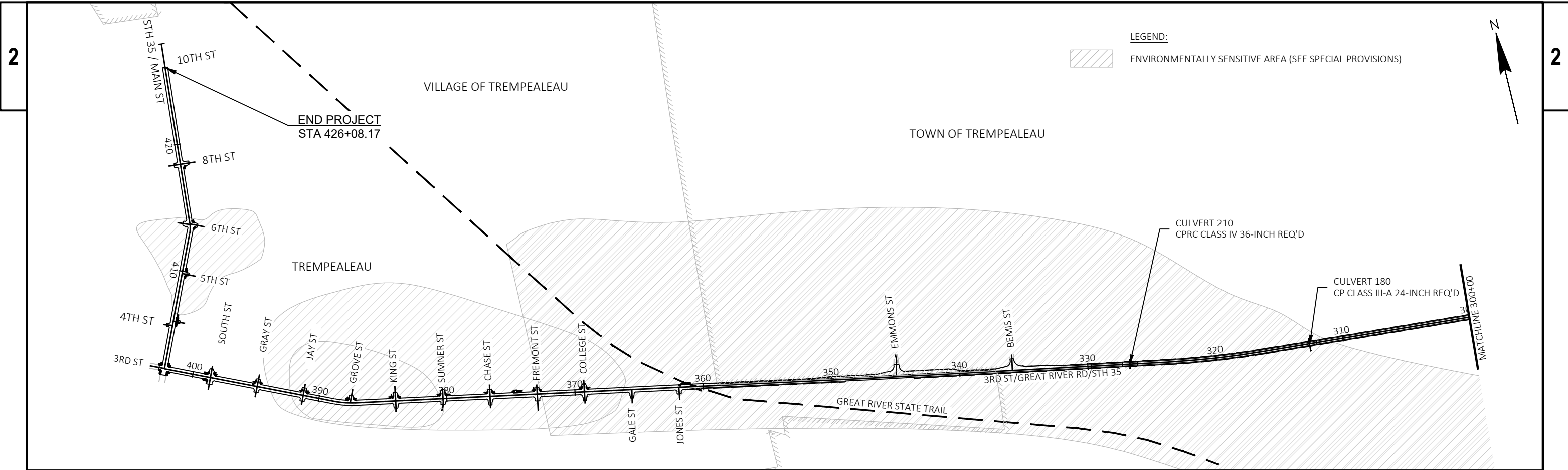


Dial 811 or (800)242-8511
www.DiggersHotline.com

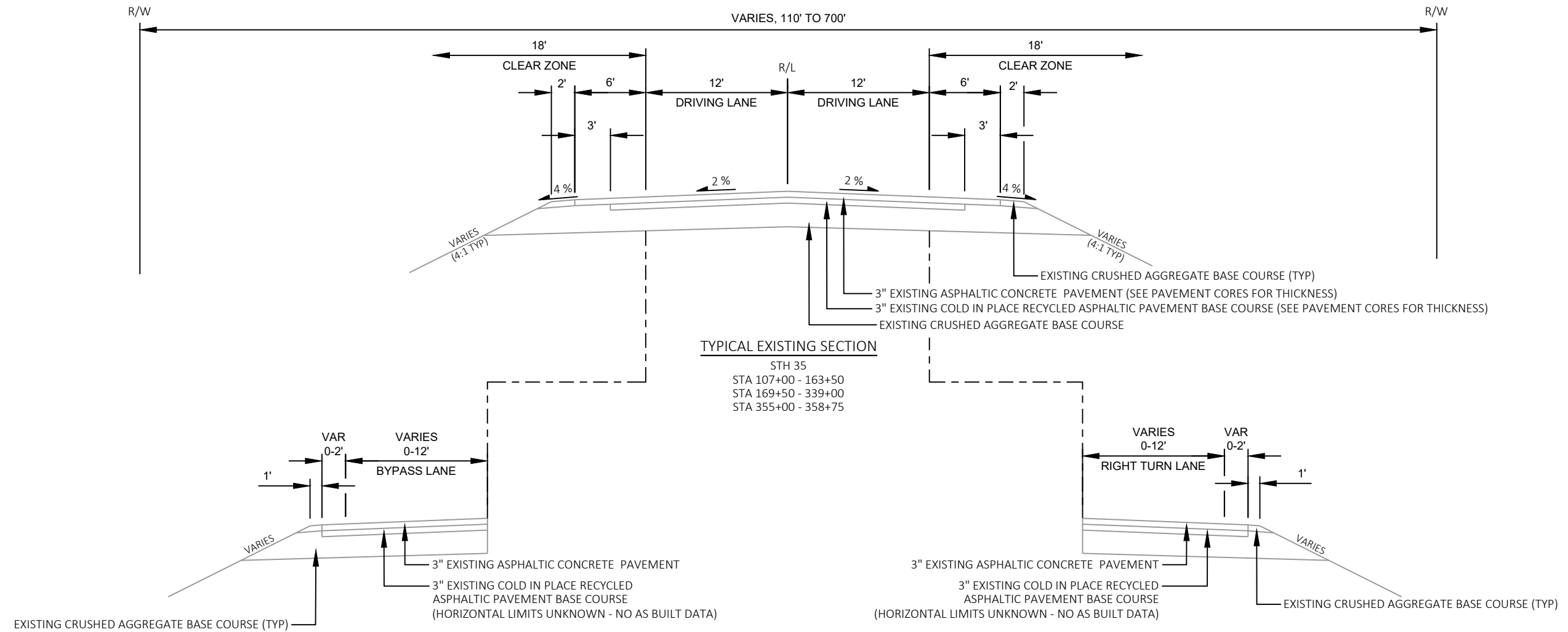
PAVEMENT CORE TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ASPHALT THICKNESS (IN)
C-24	110+92	7' RT	305832	852165.5	8
C-23	121+53	6' LT	305880.1	851105.3	8.63
C-25	132+07	7' RT	305955.2	850053.6	8.38
C-22	142+62	6' LT	306004.2	848999.9	7.13
C-26	153+16	7' RT	306078.7	847948	7.38
C-21	162+72	6' LT	306122.5	846993.6	5.13
C-27	174+26	8' RT	306203.6	845841.9	7.25
C-20	184+86	6' LT	306252.5	844783.2	9.88
C-28	195+40	8' RT	306327.8	843731.5	7
C-19	205+09	5' LT	306371.9	842763.4	7.75
C-29	217+15	8' RT	306455.9	841559.9	7.75
C-18	227+06	6' LT	306500.2	840570.6	8.38
C-30	237+64	7' RT	306575.6	839515.2	7.5
C-10	247+91	7' LT	306613	838488.1	8.75
C-11	259+45	7' RT	306637.4	837334.5	7.75
C-09	269+29	7' LT	306638.7	836350.5	8.25
C-12	280+40	7' RT	306659.8	835240.2	9.13
C-08	290+08	7' LT	306648.6	834271.7	8.13
C-13	300+92	6' RT	306662.6	833187.8	9.88
C-07	310+71	6' LT	306647.7	832209.5	8.5
C-14	322+98	9' RT	306691.7	830984.2	10.25
C-06	334+02	7' LT	306794.7	829885.2	9.63
C-15	342+45	19' RT	306911.2	829049.4	5.75
C-05	352+56	5' LT	307000.4	828041.8	8.75
C-04	367+81	5' LT	307174.5	826527.2	4
C-16	374+12	7' RT	307258.7	825901.4	3.75
C-17	385+79	7' RT	307392.8	824742.6	3.88
C-03	392+84	9' LT	307570.9	824064.5	4.88
C-02	406+83	4' RT	308332.7	823341.9	4.63
C-01	415+87	7' LT	309200	823556.2	4

STH 35					
SUPERELEVATION TRANSITION EVENT POINTS		RATE			
LOCATION	STATION	LEFT OF CROWNLIN		RIGHT OF CROWNLIN	
		LEFT SHOULDER	LEFT LANE	RIGHT LANE	RIGHT SHOULDER
END NORMAL SHOULDER	314+78.45	-4.00%	-2.00%	-2.00%	-4.00%
END NORMAL CROWN	315+31.45	-2.00%	-2.00%	-2.00%	-4.00%
LEVEL CROWN	315+84.45	.00%	.00%	-2.00%	-4.00%
REVERSE CROWN	316+37.79	2.00%	2.00%	-2.00%	-4.00%
BEGIN FULL SUPER	316+48.45	2.40%	2.40%	-2.40%	-4.00%
END FULL SUPER	323+58.71	2.40%	2.40%	-2.40%	-4.00%
REVERSE CROWN	323+69.38	2.00%	2.00%	-2.00%	-4.00%
LEVEL CROWN	324+22.71	.00%	.00%	-2.00%	-4.00%
BEGIN NORMAL CROWN	324+75.71	-2.00%	-2.00%	-2.00%	-4.00%
BEGIN NORMAL SHOULDER	325+28.71	-4.00%	-2.00%	-2.00%	-4.00%

ALL OTHER CURVES ON THE PROJECT SHALL MATCH EXISTING SUPER ELEVATIONS AND TRANSITIONS.



PROJECT NO: 7140-00-70	HWY: STH 35	COUNTY: TREMPEALEAU	PROJECT OVERVIEW	SHEET	E
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TYPICAL EXISTING SECTION

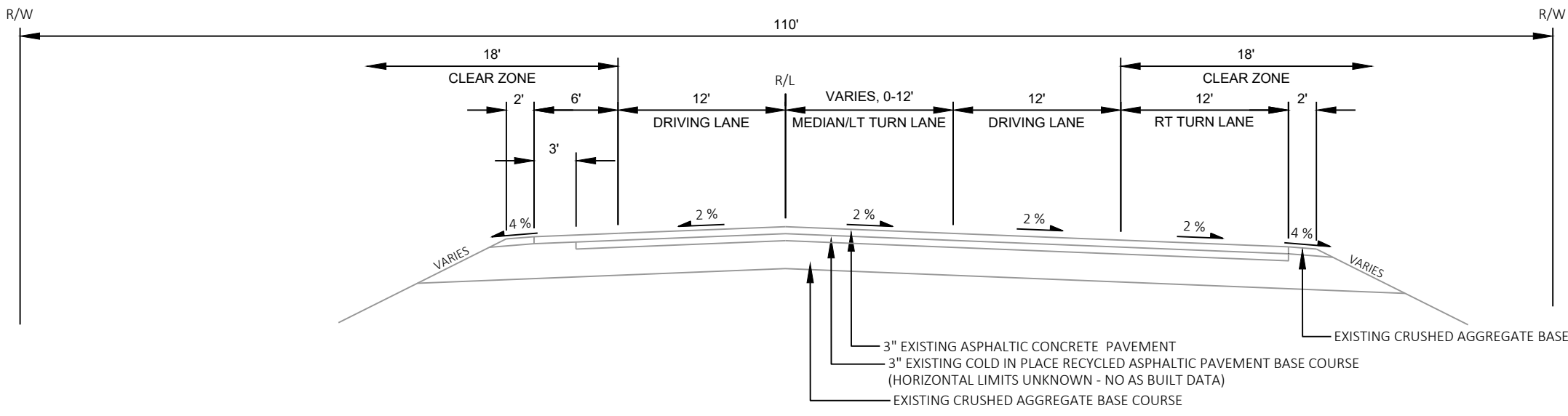
STH 35  
STA 107+00 - 163+50  
STA 169+50 - 339+00  
STA 355+00 - 358+75

TYPICAL EXISTING PARTIAL SECTION - BYPASS LANE

STH 35  
STA 210+76 - 216+70 LT  
STA 242+28 - 247+28 LT

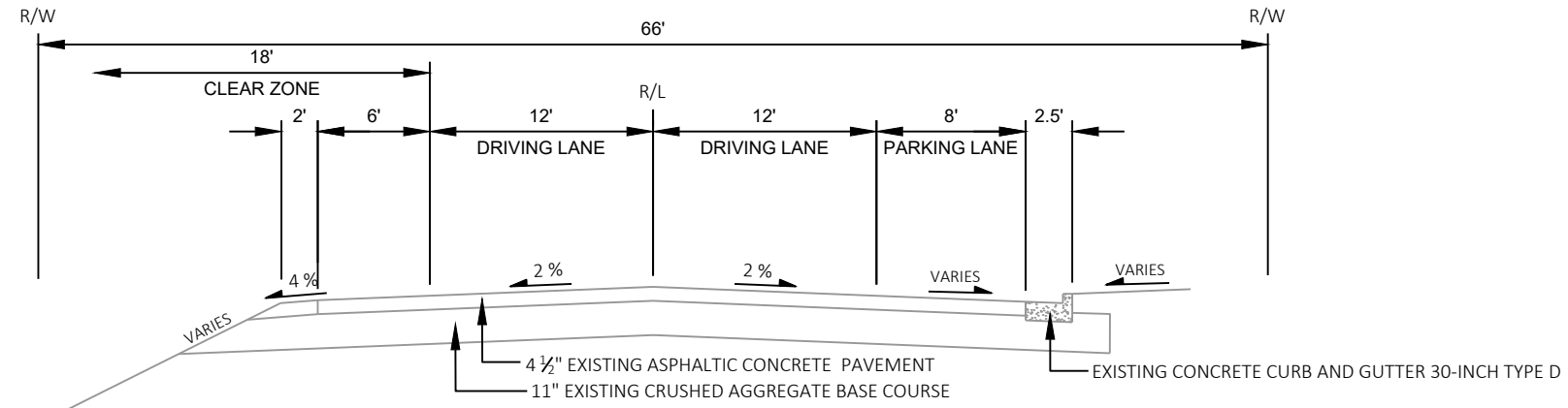
TYPICAL EXISTING PARTIAL SECTION - RIGHT TURN LANE

STH 35  
STA 210+36 - 213+66 RT  
STA 332+25 - 335+13 RT



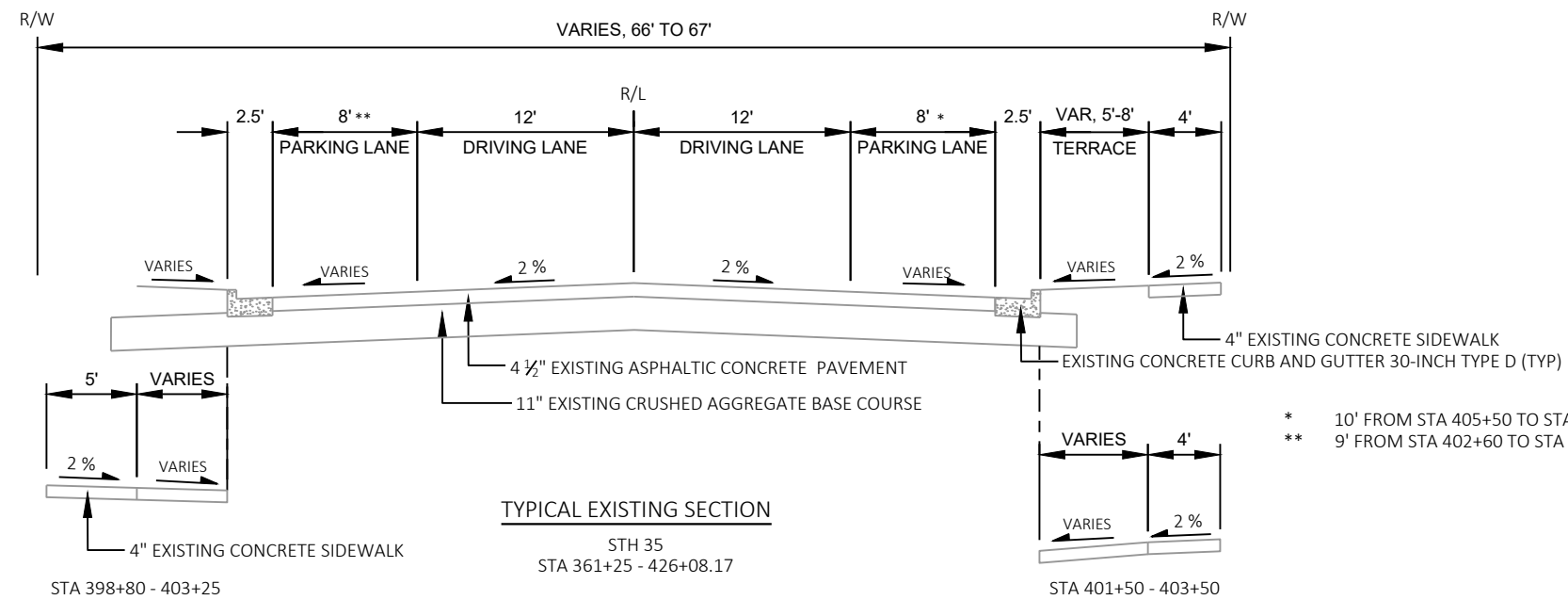
TYPICAL EXISTING SECTION

STA 339+00 - 355+00



TYPICAL EXISTING SECTION

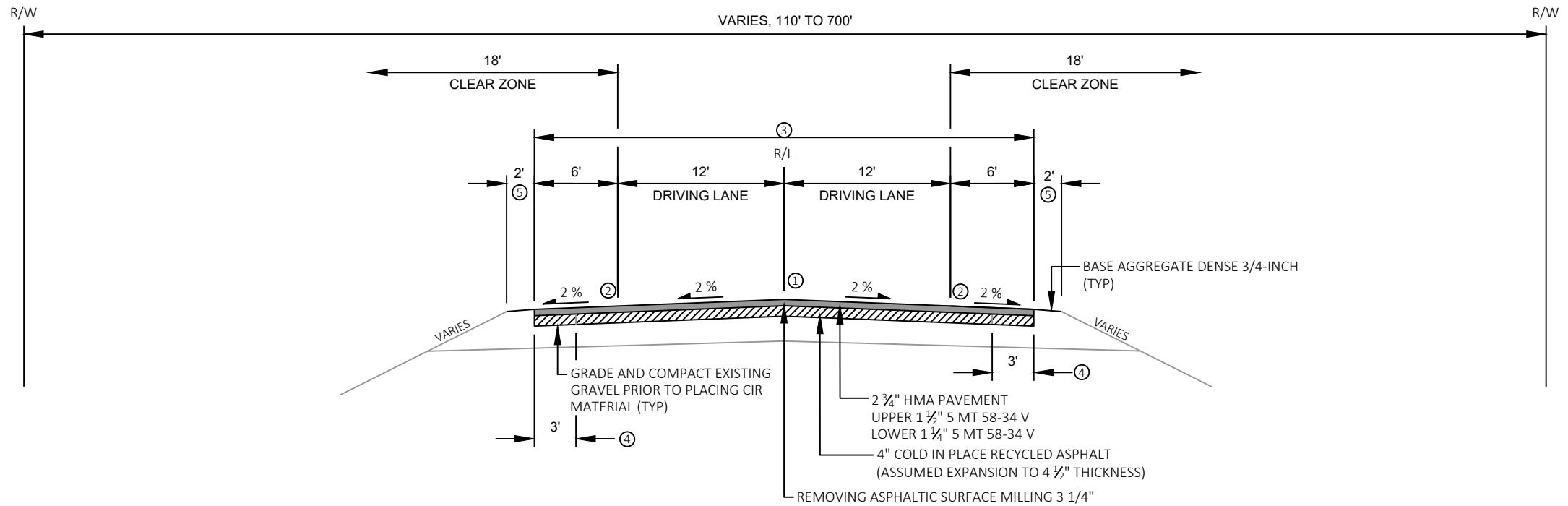
STH 35  
STA 358+75 - 361+25



TYPICAL EXISTING SECTION

STH 35  
STA 361+25 - 426+08.17

\* 10' FROM STA 405+50 TO STA 413+50  
\*\* 9' FROM STA 402+60 TO STA 413+50



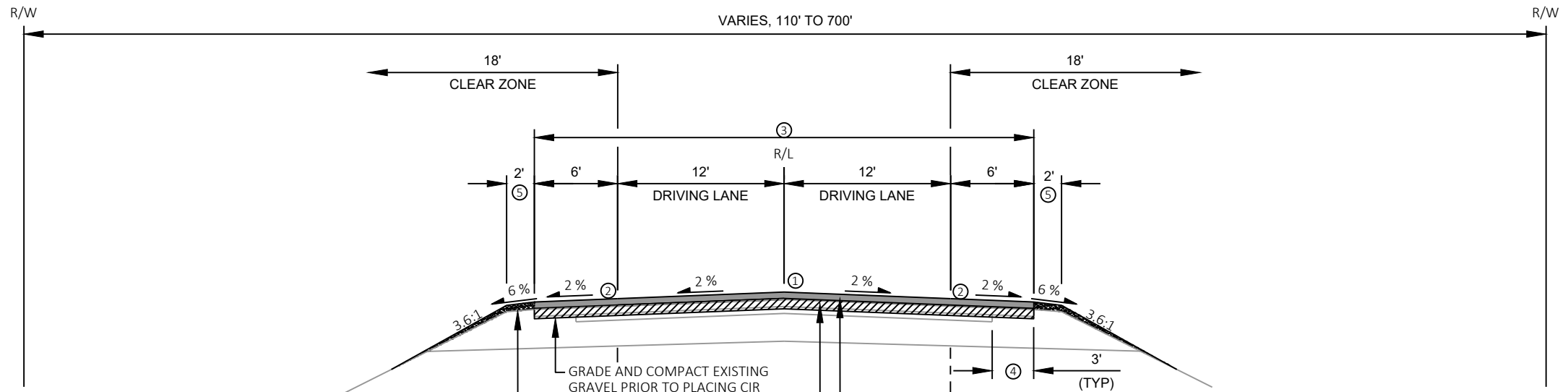
TYPICAL FINISHED SECTION

STH 35  
STA 107+00 - 163+50

LEGEND:

- ① ASPHALTIC CENTERLINE RUMBLE STRIPS 2-LANE RURAL. SEE MISCELLANEOUS QUANTITIES FOR LOCATIONS.
- ② ASPHALTIC SHOULDER RUMBLE STRIPS 2-LANE RURAL. SEE MISCELLANEOUS QUANTITIES FOR LOCATIONS.
- ③ "PREPARE FOUNDATION FOR ASPHALTIC PAVING" & "PREPARE FOUNDATION FOR CIR LAYER" PAY LIMITS.
- ④ 4" OF EXISTING BASE AGGREGATE INCLUDED IN COLD IN-PLACE RECYCLING. PREPARING FOUNDATION INCLUDED IN BID ITEM "PREPARE FOUNDATION FOR CIR LAYER"
- ⑤ SHAPING SHOULDERS

COLD IN-PLACE RECYCLING LIMITS

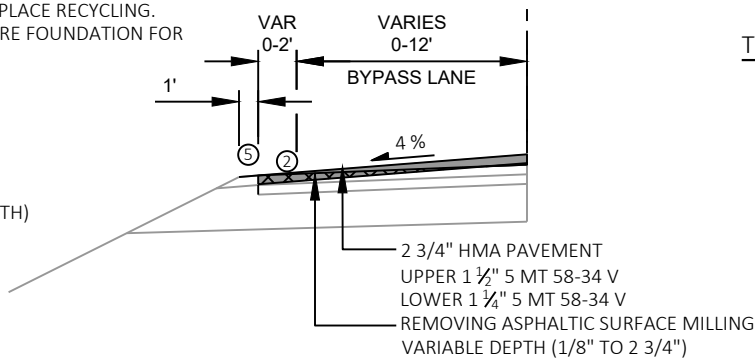


\*SHOULDER SLOPE TRANSITIONS FROM 6% TO 2%. SEE BUTT JOINT DETAIL.

**LEGEND:**

- ① ASPHALTIC CENTERLINE RUMBLE STRIPS 2-LANE RURAL. SEE MISCELLANEOUS QUANTITIES FOR LOCATIONS.
- ② ASPHALTIC SHOULDER RUMBLE STRIPS 2-LANE RURAL. SEE MISCELLANEOUS QUANTITIES FOR LOCATIONS.
- ③ "PREPARE FOUNDATION FOR ASPHALTIC PAVING" & "PREPARE FOUNDATION FOR CIR LAYER" PAY LIMITS.
- ④ 1" OF EXISTING BASE AGGREGATE INCLUDED IN COLD IN-PLACE RECYCLING. PREPARING FOUNDATION INCLUDED IN BID ITEM "PREPARE FOUNDATION FOR CIR LAYER"
- ⑤ SHAPING SHOULDERS

- COLD IN-PLACE RECYCLING LIMITS
- REMOVING ASPHALTIC SURFACE MILLING (VARIABLE DEPTH)

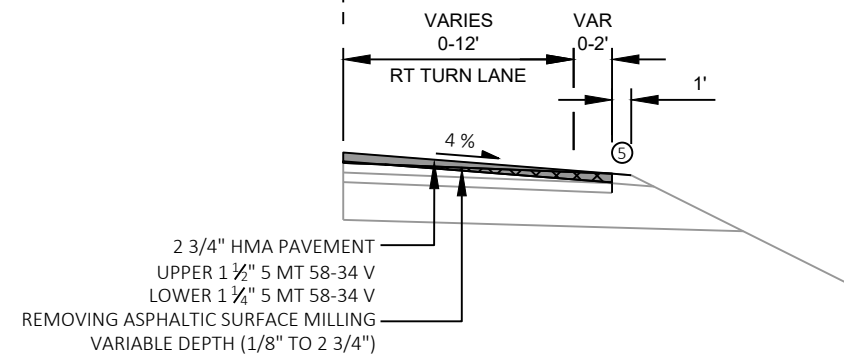


**TYPICAL FINISHED PARTIAL SECTION - BYPASS LANE**

STH 35  
 STA 210+76 - 216+70 LT  
 STA 242+28 - 247+28 LT

**TYPICAL FINISHED SECTION**

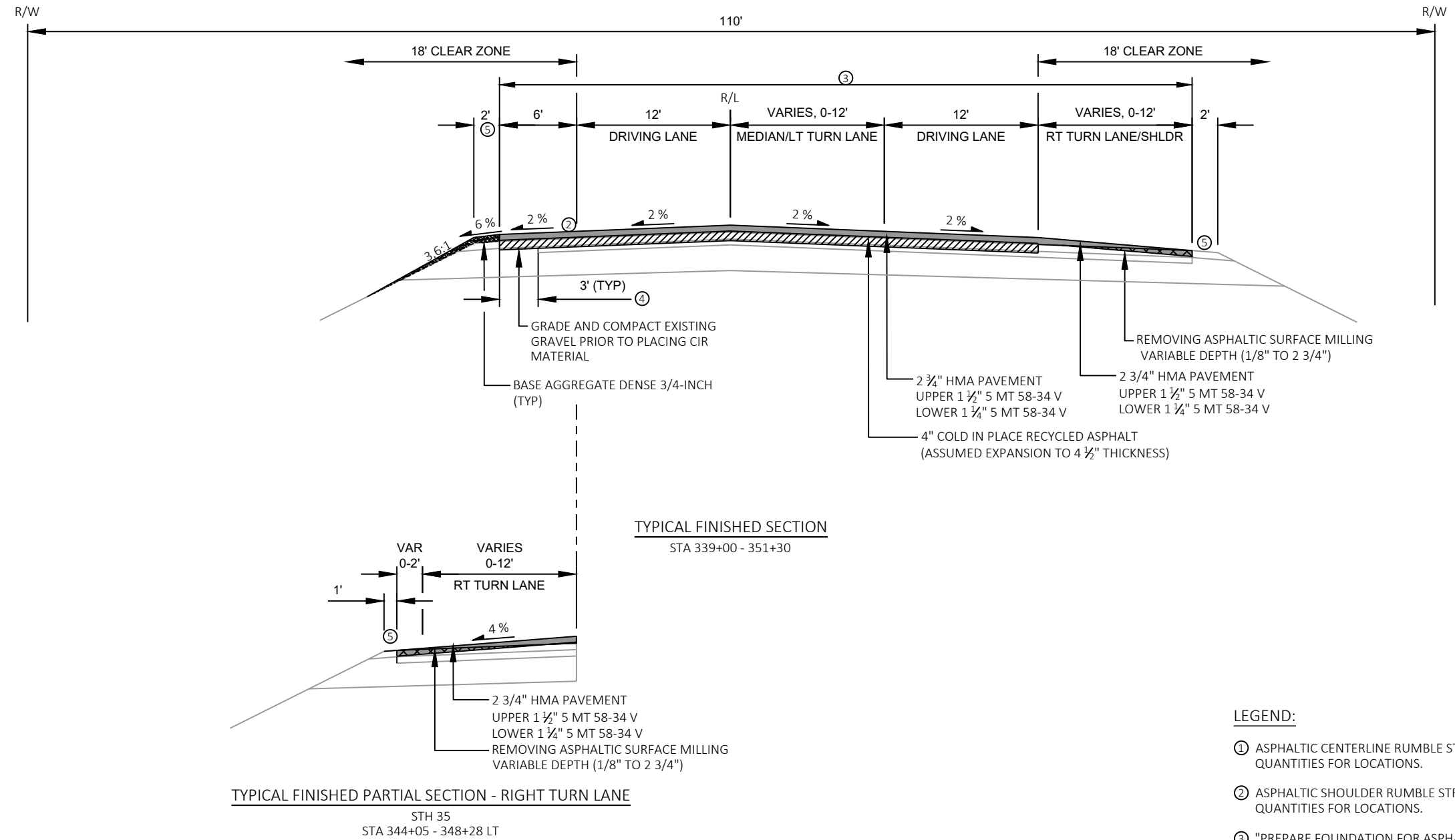
STH 35  
 STA 169+50 - 339+00  
 STA 351+30 - 358+75



**TYPICAL FINISHED PARTIAL SECTION - RIGHT TURN LANE**

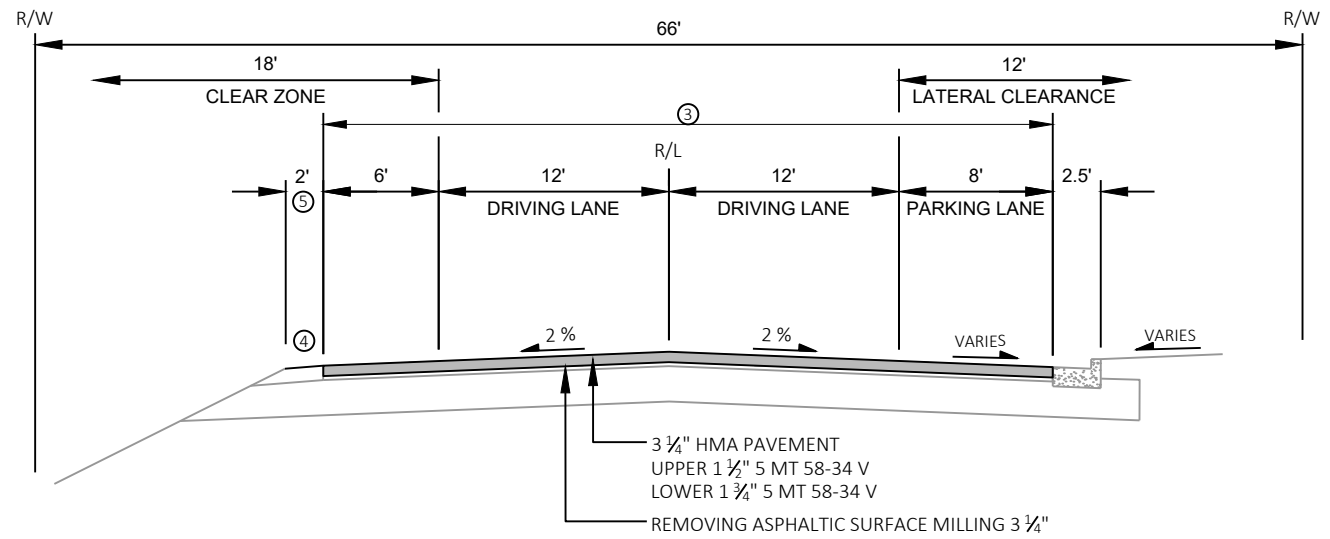
STH 35  
 STA 210+36 - 213+66 RT  
 STA 332+25 - 335+13 RT





- LEGEND:**
- ① ASPHALTIC CENTERLINE RUMBLE STRIPS 2-LANE RURAL. SEE MISCELLANEOUS QUANTITIES FOR LOCATIONS.
  - ② ASPHALTIC SHOULDER RUMBLE STRIPS 2-LANE RURAL. SEE MISCELLANEOUS QUANTITIES FOR LOCATIONS.
  - ③ "PREPARE FOUNDATION FOR ASPHALTIC PAVING" & "PREPARE FOUNDATION FOR CIR LAYER" PAY LIMITS.
  - ④ 1" OF EXISTING BASE AGGREGATE INCLUDED IN COLD IN-PLACE RECYCLING. PREPARING FOUNDATION INCLUDED IN BID ITEM "PREPARE FOUNDATION FOR CIR LAYER"
  - ⑤ SHAPING SHOULDERS

- COLD IN-PLACE RECYCLING LIMITS
- REMOVING ASPHALTIC SURFACE MILLING (VARIABLE DEPTH)

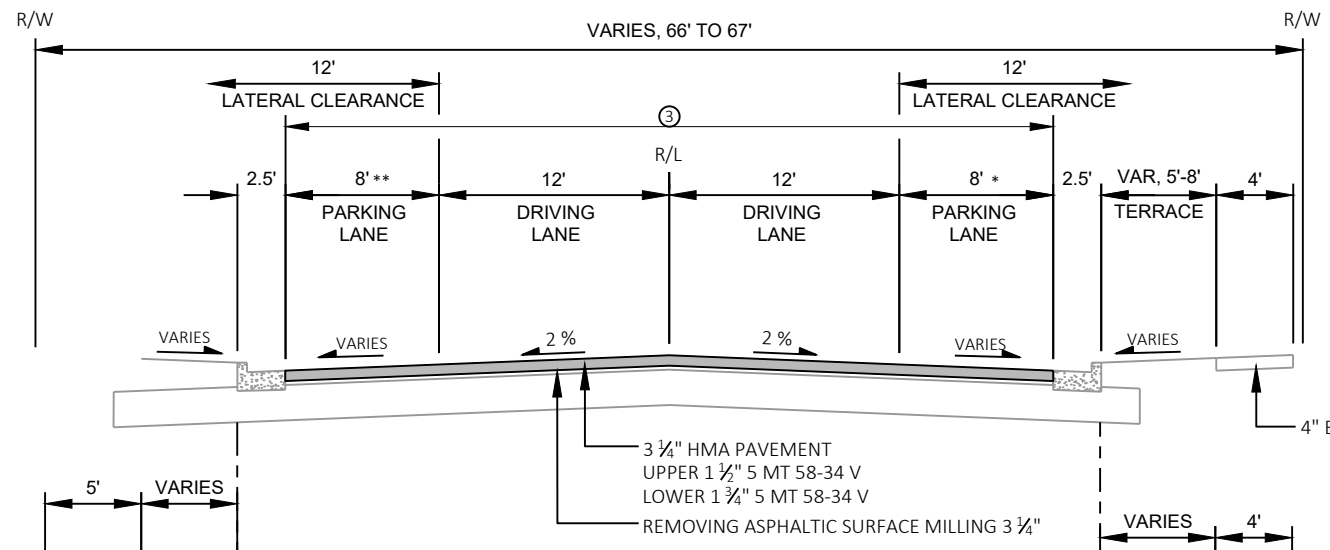


TYPICAL FINISHED SECTION  
 STH 35  
 STA 358+75 - 361+25

LEGEND:

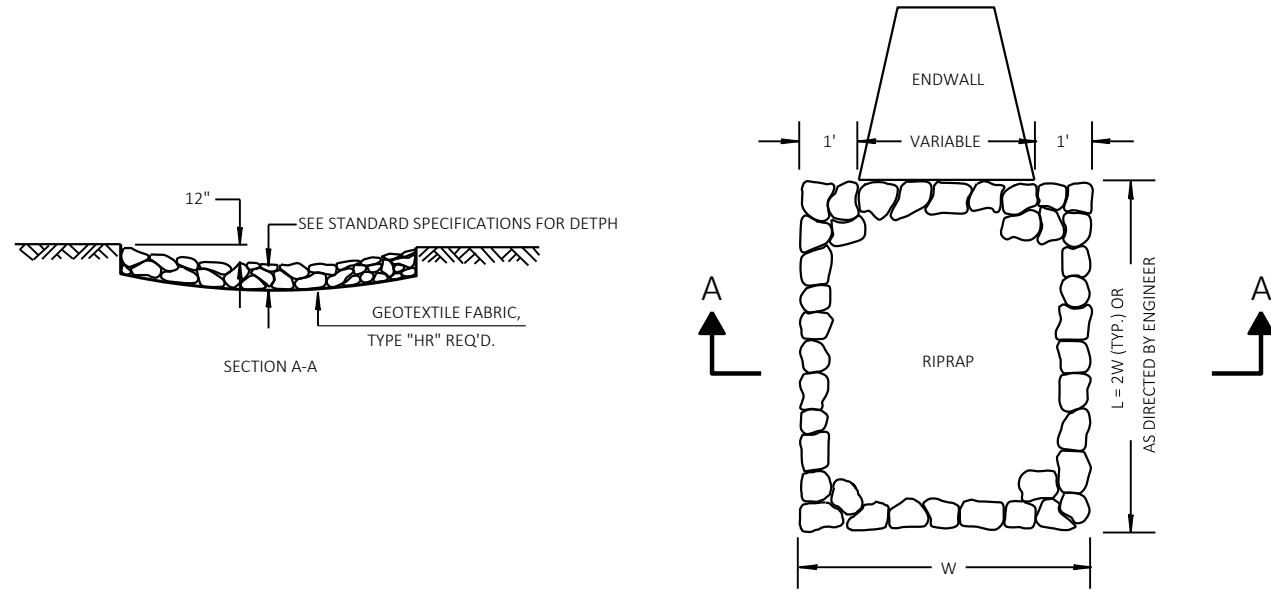
- ① ASPHALTIC CENTERLINE RUMBLE STRIPS 2-LANE RURAL. SEE MISCELLANEOUS QUANTITIES FOR LOCATIONS.
- ② ASPHALTIC SHOULDER RUMBLE STRIPS 2-LANE RURAL. SEE MISCELLANEOUS QUANTITIES FOR LOCATIONS.
- ③ "PREPARE FOUNDATION FOR ASPHALTIC PAVING" PAY LIMITS
- ④ ADDITIONAL BASE AGGREGATE DENSE 3/4 -INCH MAY BE REQUIRED TO CONSTRUCT TO ELEVATION OF FINISHED HMA PAVEMENT AND AS DIRECTED BY THE ENGINEER.
- ⑤ SHAPING SHOULDERS

CURB RAMP CONSTRUCTION WILL TAKE PLACE AT THE INTERSECTIONS OF COLLEGE, FREMONT, CHASE, SUMNER, KING, GROVE, JAY, GRAY, SOUTH, MAIN, 4TH, 5TH, PINE, 8TH ST INTERSECTIONS AND THE GREAT RIVER STATE TRAIL CROSSING. SEE CURB RAMP DETAILS FOR MORE INFORMATION.

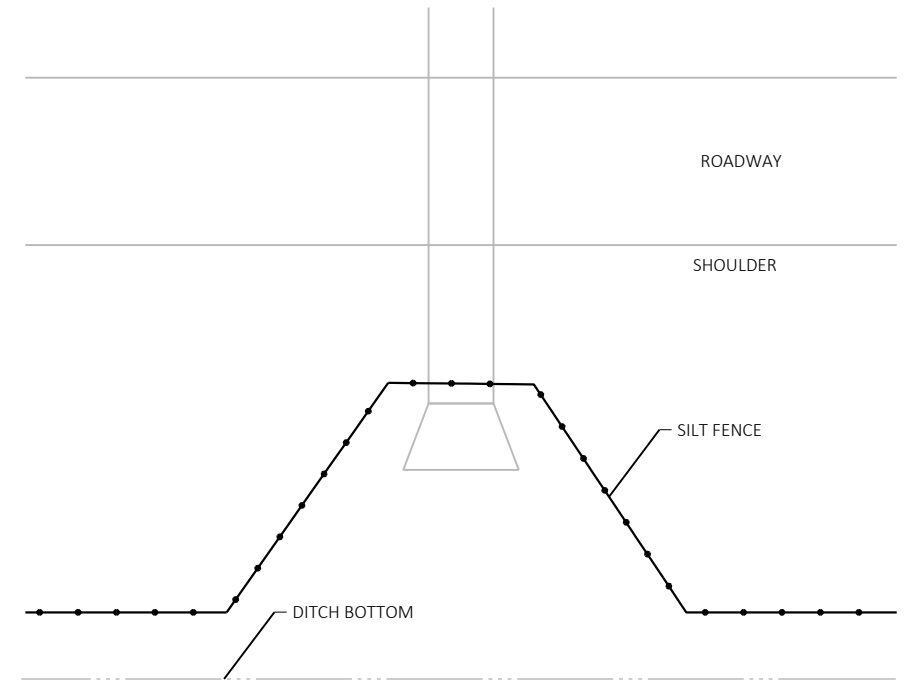


TYPICAL FINISHED SECTION  
 STH 35  
 STA 398+80 - 403+25

\* 10' FROM STA 405+50 TO STA 413+50  
 \*\* 9' FROM STA 402+60 TO STA 413+50

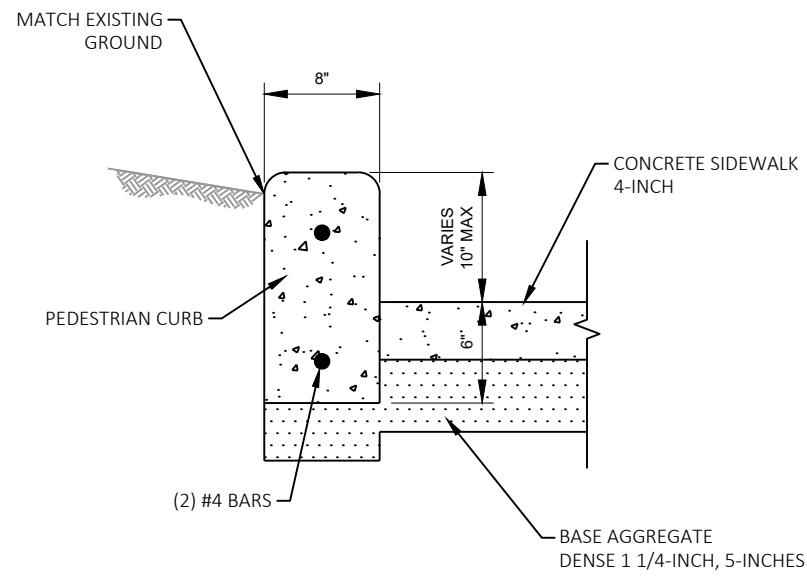


**RIPRAP TREATMENT AT CULVERTS**



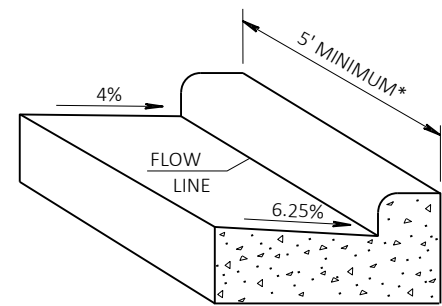
**TYPICAL SILT FENCE DETAIL AT PIPE INLET**

NOTE  
SEE SILT FENCE SDD FOR ADDITIONAL INFORMATION.

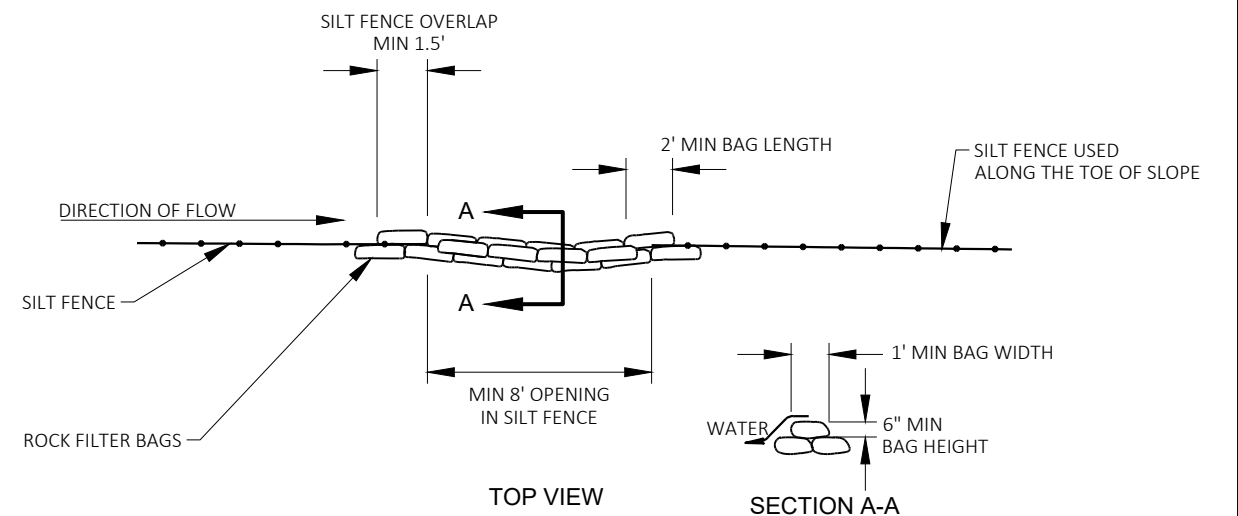


**PEDESTRIAN CURB EXPOSED HEIGHT OVER 6-INCHES**

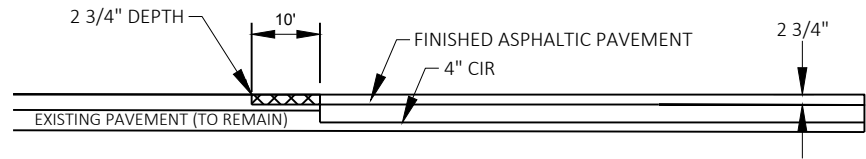
\*SEE CURB RAMP DETAILS FOR LOCATIONS & EXACT LENGTH.



**DETAIL OF GUTTER SLOPE TRANSITION**



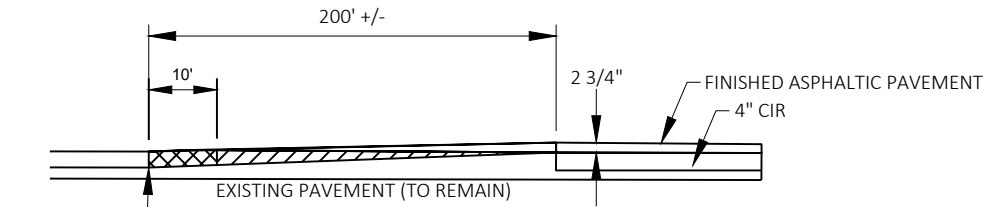
**ROCK BAGS USED FOR SILT FENCE RELIEF**



REMOVING ASPHALTIC SURFACE BUTT JOINTS

**CIR BUTT JOINT DETAIL**

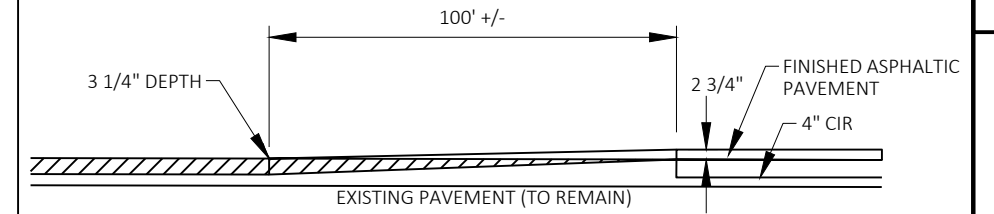
MAINLINE  
(STA 107+00 TO 109+00)  
(STA 161+50 TO 163+50)



REMOVING ASPHALTIC SURFACE MILLING   
REMOVING ASPHALTIC SURFACE BUTT JOINTS

**CIR BUTT JOINT DETAIL**

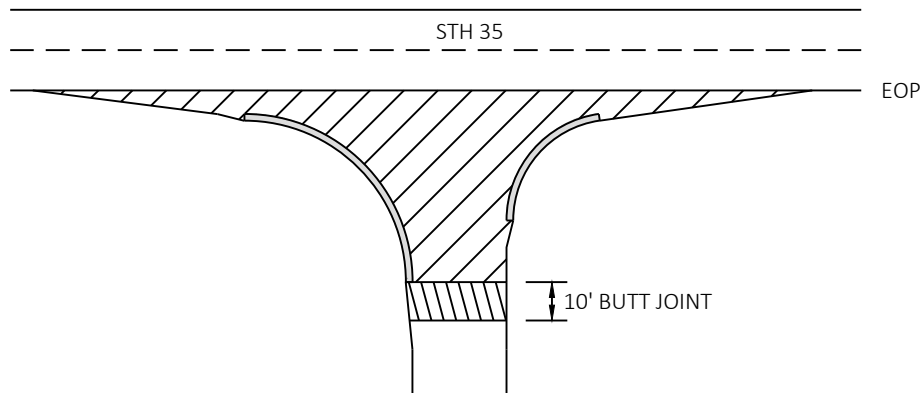
MAINLINE  
(STA 169+50 TO 171+50)



REMOVING ASPHALTIC SURFACE MILLING   
REMOVING ASPHALTIC SURFACE BUTT JOINTS

**CIR TRANSITION DETAIL**

MAINLINE  
(STA 358+75 TO 359+75)

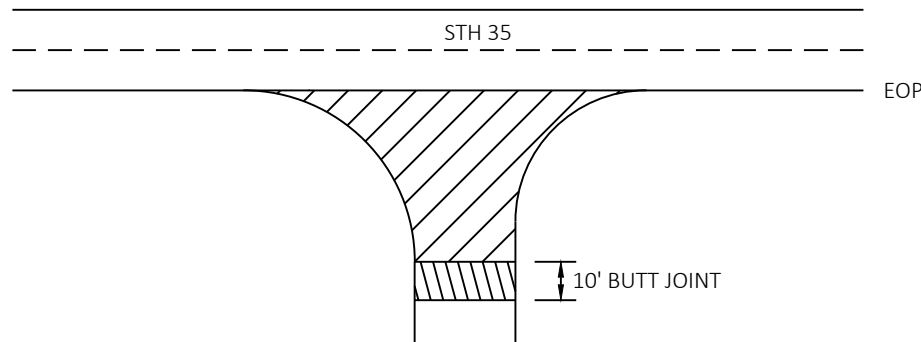


REMOVING ASPHALTIC SURFACE MILLING (VARIABLE DEPTH)  
 REMOVING ASPHALTIC SURFACE BUTT JOINTS  
SEE BUTT JOINT DETAIL

NOTE: WHEN MATCHING TO AN UNPAVED SURFACE  
BUTT JOINT IS NOT REQUIRED

**SIDE ROADS - WITH CURB AND GUTTER**

EMMONS ST  
BEMIS ST  
CTH M

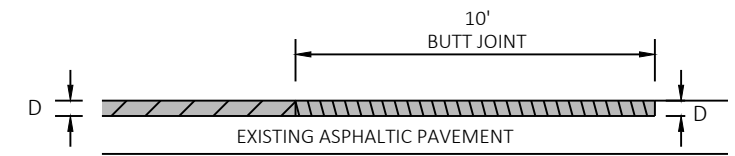


REMOVING ASPHALTIC SURFACE MILLING (VARIABLE DEPTH)  
 REMOVING ASPHALTIC SURFACE BUTT JOINTS  
SEE BUTT JOINT DETAIL

NOTE: WHEN MATCHING TO AN UNPAVED SURFACE  
BUTT JOINT IS NOT REQUIRED

**SIDE ROADS - WITHOUT CURB AND GUTTER**

MCDONAH LN  
SOUTH ST



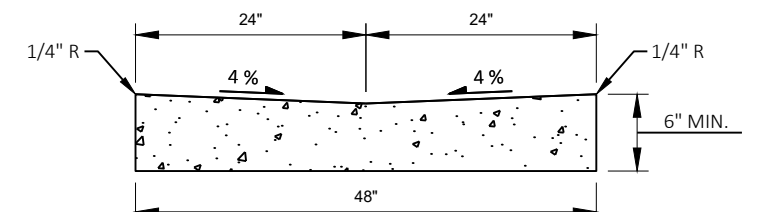
HMA PAVEMENT  
 REMOVING ASPHALTIC SURFACE MILLING  
 REMOVING ASPHALTIC SURFACE BUTT JOINTS

\*8TH ST, 6TH ST, 5TH ST, 4TH ST, 3RD CT, MAIN ST, 3RD ST, SOUTH ST, GRAY ST, JAY ST, GROVE ST, KING ST, SUMNER ST, CHASE ST, FREMONT ST, COLLEGE ST, GALE ST & JONES ST

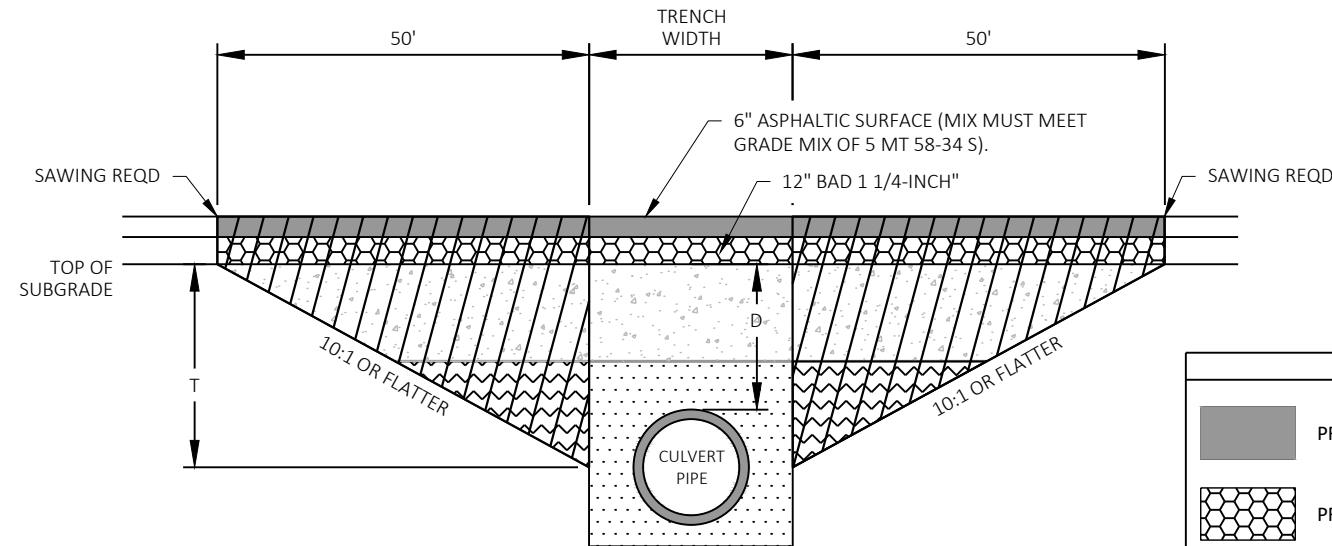
D = 3.25" END PROJECT LIMITS  
3.25" SIDE ROADS\*  
2.75" SIDE ROADS \*\*

\*\*EMMONS ST, BEMIS ST, SOUTH ST, MCDONAH ST & CTH M

**BUTT JOINT**  
SIDE ROADS  
END PROJECT LIMITS









**CONCRETE GUTTER 48-INCH**

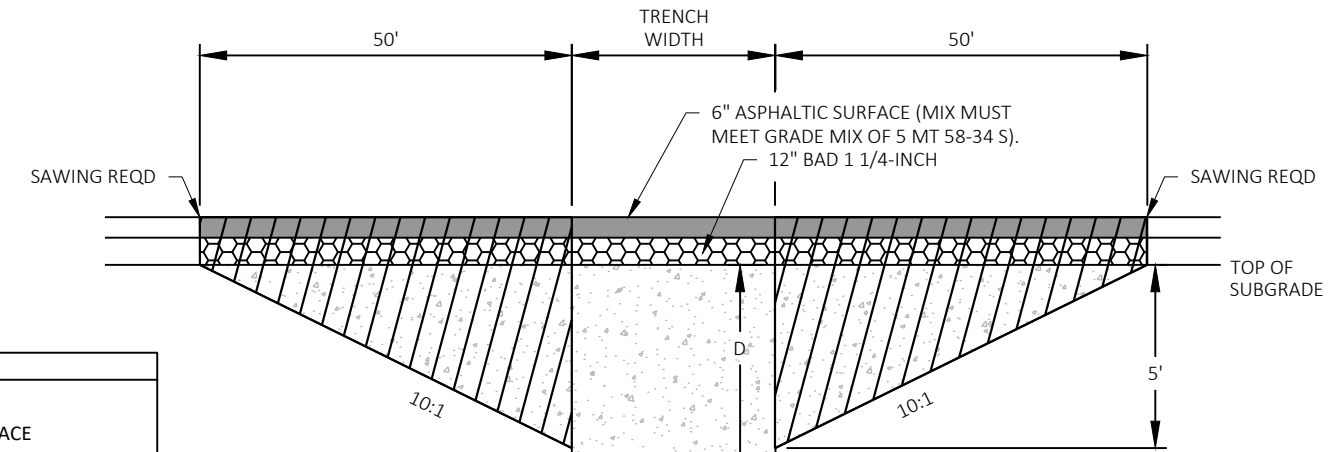


DEPTH D < 6 FT

TRANSITION CUT DEPTH (T) = THE LESSER OF DEPTH TO CENTER OF PIPE OR 5 FT.  
DO NOT EXTEND TRANSITION CUT BELOW HORIZONTAL CENTER OF PIPE.

KEY

-  PROPOSED SURFACE
-  PROPOSED BASE
-  TRENCH BACKFILL
-  TRENCH OR FOUNDATION BACKFILL
-  FOUNDATION BACKFILL
-  TRANSITION CUT



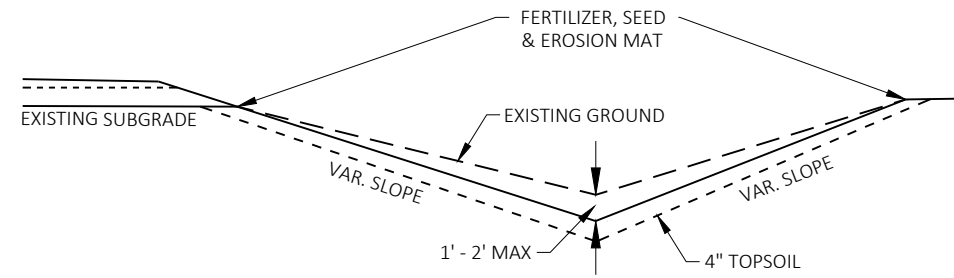
DEPTH D ≥ 6 FT

**NOTES**

- TRANSITION CUT IS PAID AS EXCAVATION COMMON.
- TRANSITION CUT WIDTH IS FROM SUBGRADE SHOULDER POINT TO SUBGRADE SHOULDER POINT.
- BACKFILL THE TRANSITION CUT AREAS WITH FOUNDATION AND TRENCH BACKFILL AS SPECIFIED IN STANDARD SPEC 520.
- PERFORM CULVERT PIPE INSTALLATION BEFORE *MILLING AND PAVING*.
- PLACE 465.0105 ASPHALTIC SURFACE AFTER CULVERT PIPE INSTALLATION AND BEFORE MILLING OPERATION.
- DEPTH LISTED IN TABLE BELOW IS MEASURED AT THE ALIGNMENT CROSSING.

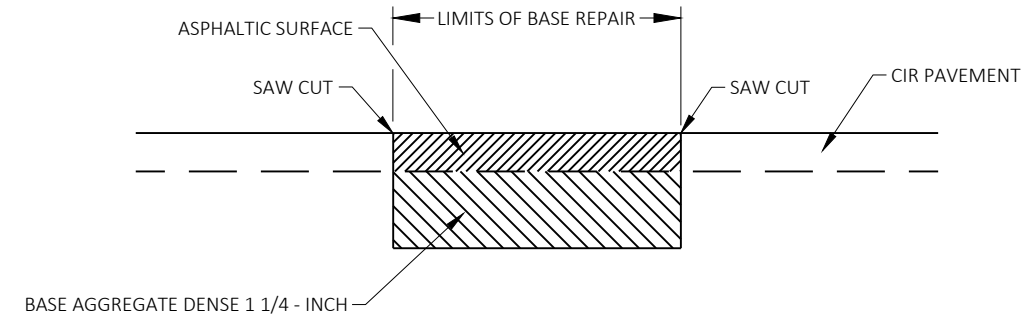
**CULVERT PIPE TRANSITION**

ROUTE	STA (CL)	DEPTH D (FT)	PIPE DIA (IN)	REMARKS
STH 35	312+63	5.9	24	CULVERT 180
STH 35	326+73	1.6	36	CULVERT 210



**DITCH CLEANING DETAIL**

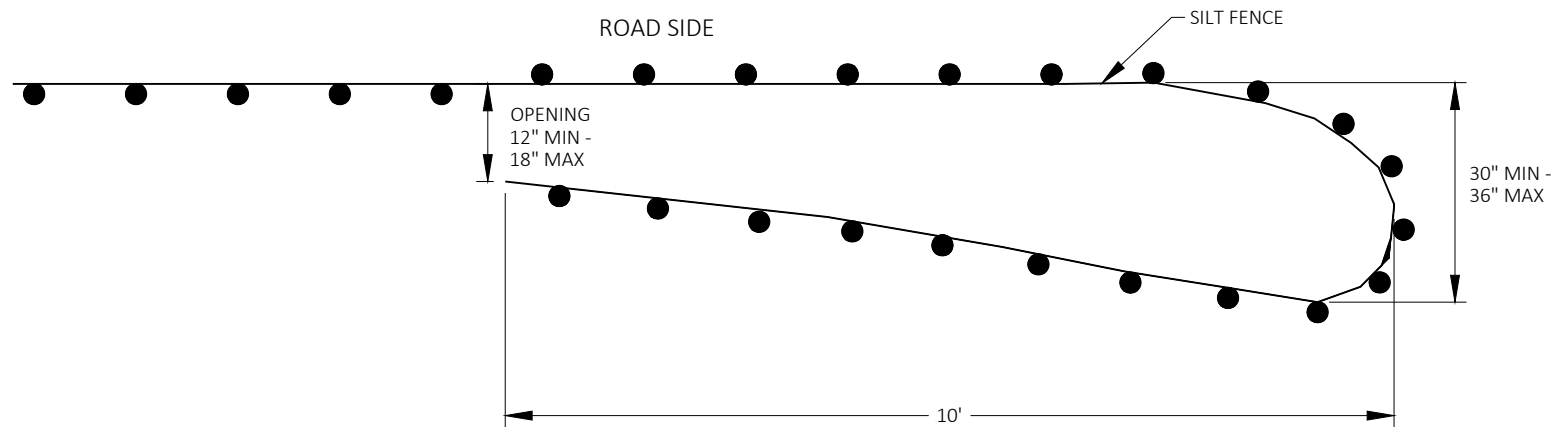
- 211+50 - 213+00 RT
- 224+50 - 225+50 LT/RT
- 230+50 - 232+50 LT
- 260+06 - 262+06, LT
- 260+81 - 262+06 RT



PRIOR TO CIR PROCESS  
(COVERED UNDER BID ITEM "PREPARATION OF FOUNDATION FOR CIR PAVEMENT")

POST CIR PROCESS  
(COVERED UNDER BID ITEM "PREPARE FOUNDATION FOR HMA PAVEMENT")

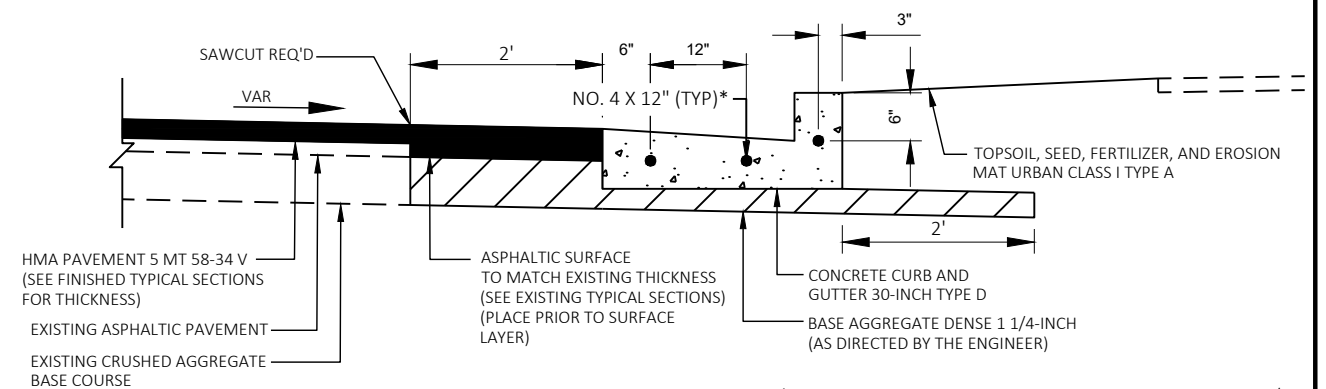
**BASE REPAIR FOR CIR PAVEMENT**



PLAN VIEW

**AMPHIBIAN AND REPTILE EXCLUSION TURN-AROUND**

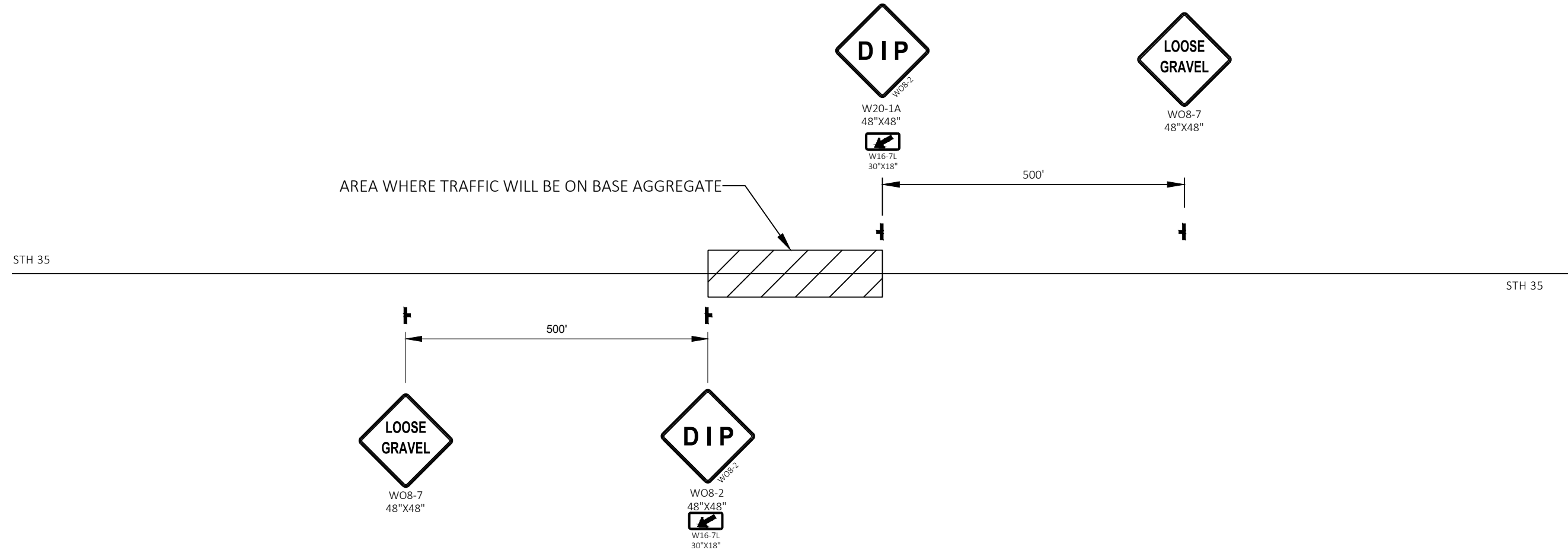
GENERAL NOTES:  
THE PURPOSE OF THE SMALL ANIMAL TURN AROUNDS ARE TO REDIRECT THE TURTLES AND SNAKES AWAY FROM THE ROADWAY. PLACE FENCE POSTS ON THE UP SLOPE SIDE OF THE FENCE. TURTLE BARRIER POSTS FOR THE TURN-AROUND SHOULD BE ON THE INSIDE OF THE TURN-AROUND. 5 POSTS MINIMUM WITHIN THE ARC OF TURN-AROUND.



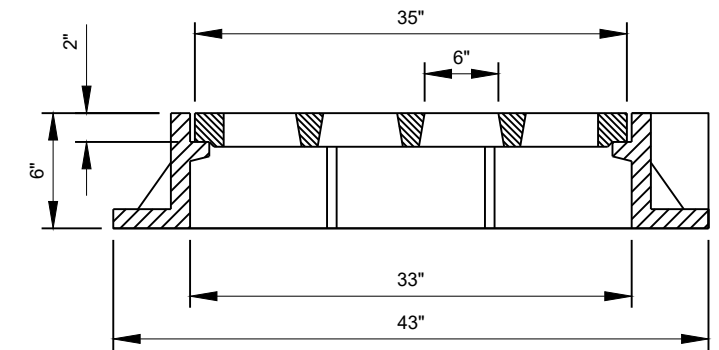
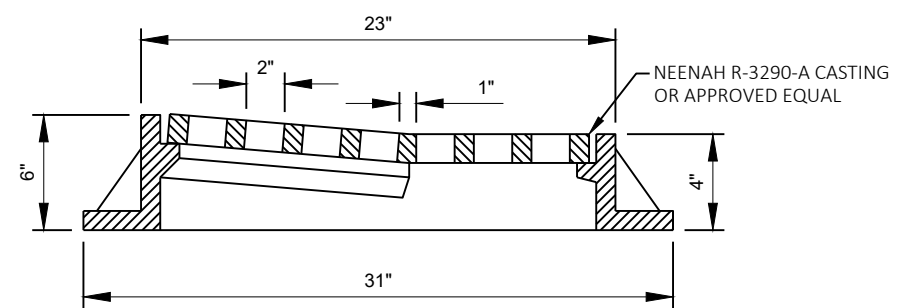
\*INSTALL DRILLED TIE BARS WHEN MATCHING EXISTING C/G

**CONCRETE CURB AND GUTTER REPLACEMENT**

SEE PLAN SHEETS FOR REPLACEMENT LOCATIONS



**TEMPORARY SIGNING AT CULVERT PIPE REPLACEMENTS**  
 USE AT ALL CULVERT PIPE REPLACEMENTS WHERE ASPHALT PATCH IS NOT PLACED THE SAME DAY



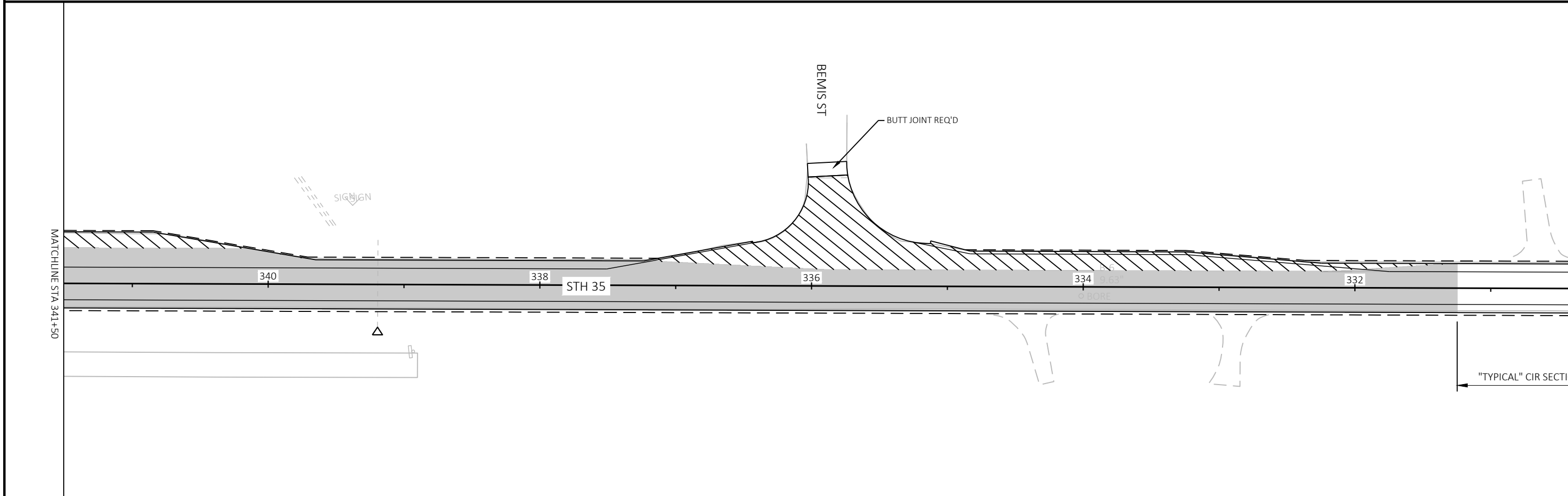
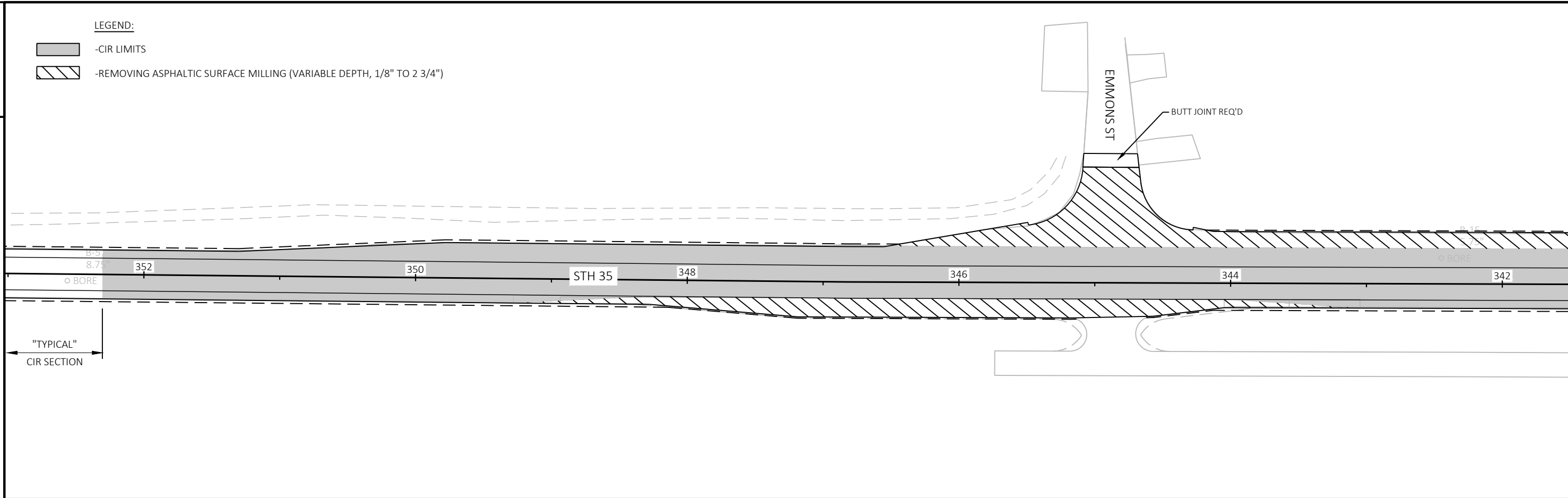
**INLET COVERS TYPE H-D DETAIL**

2

LEGEND:

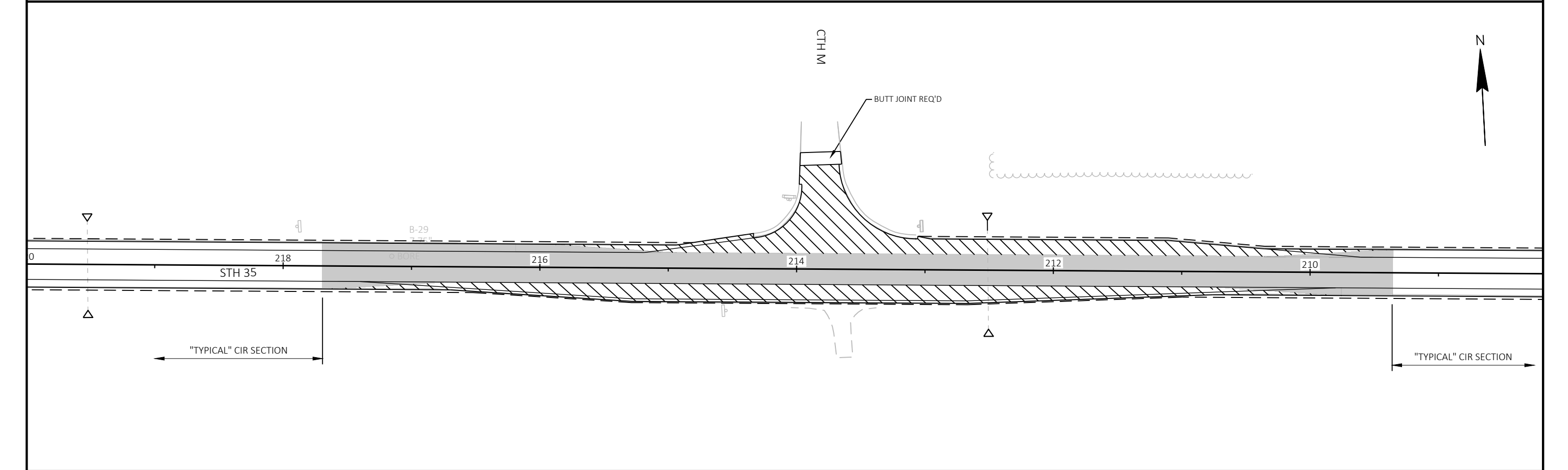
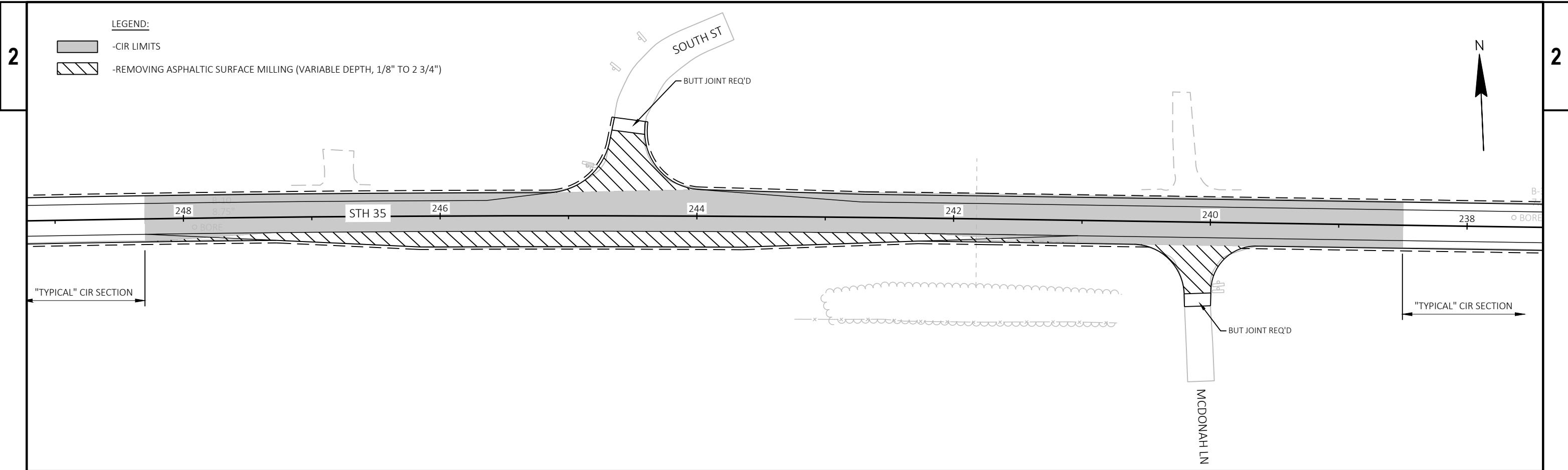
- CIR LIMITS
- REMOVING ASPHALTIC SURFACE MILLING (VARIABLE DEPTH, 1/8" TO 2 3/4")

2



PROJECT NO: 7140-00-70	HWY: STH 35	COUNTY: TREMPPEALEAU	INTERSECTION DETAILS: CIR LIMITS	SHEET	E
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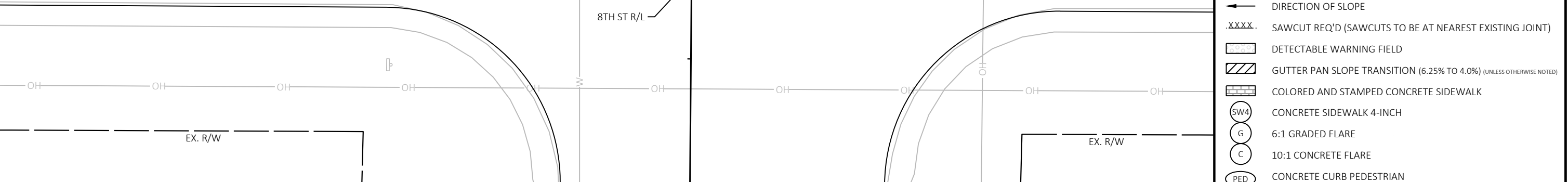


PROJECT NO: 7140-00-70      HWY: STH 35      COUNTY: TREMPLEALEAU      INTERSECTION DETAILS: CIR LIMITS      SHEET      E

0100 8TH ST					
POINT NO.	STATION	OFFSET	ELEV.	X	Y
0101	418+05.66	20.25' RT	732.38	823584.25	309418.43
0102	418+20.16	25.60' RT	732.13	823589.69	309432.90
0103	418+22.54	28.50' RT	732.09	823592.60	309435.27
0104	418+24.88	33.50' RT	732.05	823597.61	309437.58
0105	418+25.66	40.01' RT	732.00	823604.13	309438.32
0106	418+22.26	33.50' RT	732.01	823597.60	309434.96
0107	418+19.43	28.50' RT	732.03	823592.58	309432.16
0108	418+13.43	28.50' RT	732.39	823592.55	309426.16
0109	418+08.43	28.50' RT	732.46	823592.52	309421.16
0110	418+03.43	29.63' RT	732.79	823593.62	309416.15
0111	418+03.43	33.08' RT	732.72	823597.07	309416.13
0112	418+08.43	33.50' RT	732.51	823597.52	309421.13
0113	418+13.43	33.50' RT	732.44	823597.55	309426.13
0114	418+19.43	33.50' RT	732.02	823597.58	309432.13

STH 35 R/L  
419+00

MAIN ST/STH 35

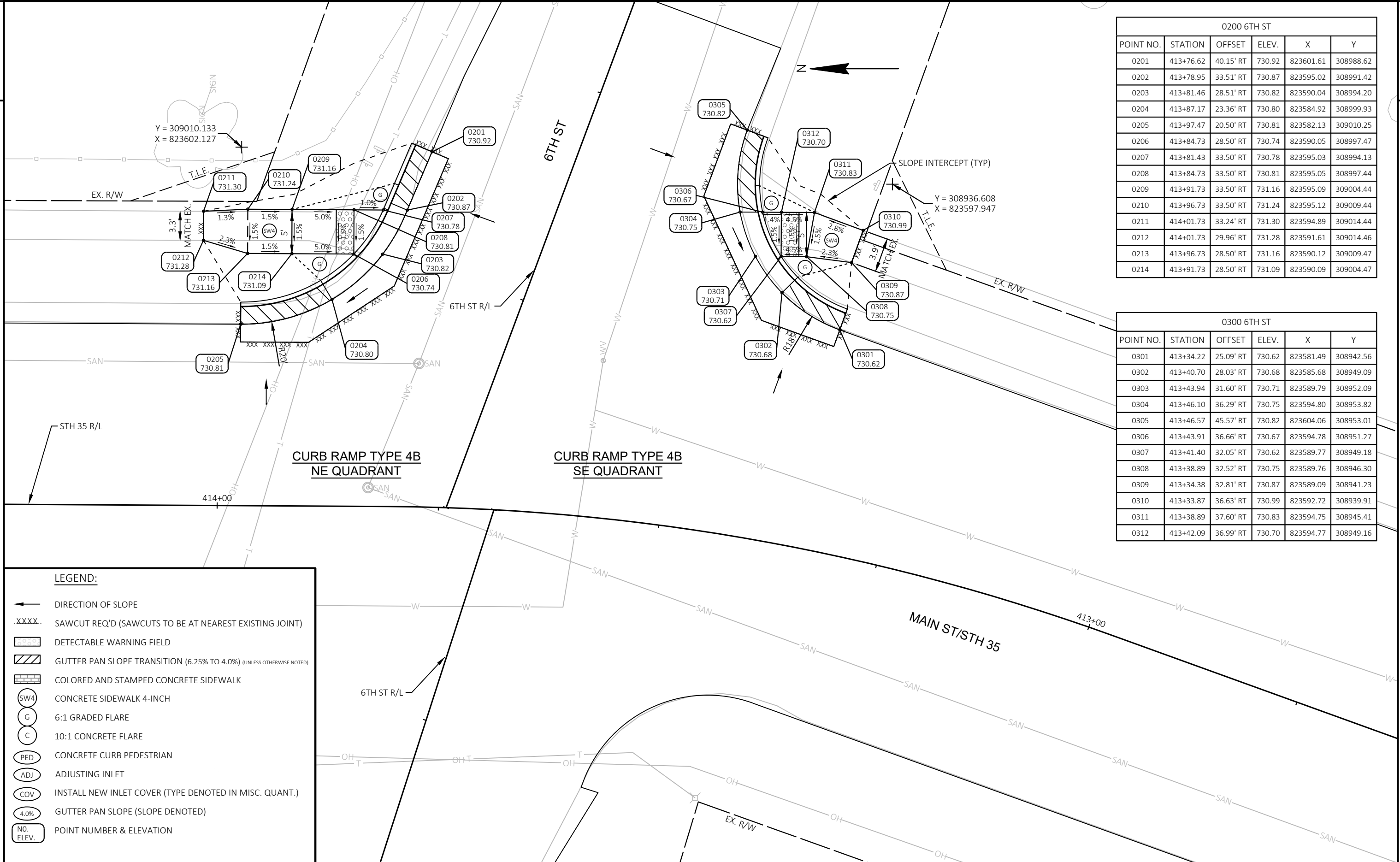


**CURB RAMP TYPE 4B  
SE QUADRANT**

- NOTES:**
1. CONTRACTOR TO FIELD VERIFY ELEVATION GRADES, SLOPES, LENGTHS AND MATCH POINTS, PRIOR TO CURB RAMP AND SIDEWALK CONSTRUCTION.
  2. THE ENGINEER MAY ADJUST FIELD CONDITIONS TO FIT FIELD CONDITION.
  3. DASHED LINES SHOWN FOR PROPOSED SIDEWALK ARE FOR INFORMATION ONLY AND DO NOT INDICATE JOINT LOCATIONS.
  4. SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS.
  5. ALL STATION AND OFFSET INFORMATION REFERENCE STH 35 ALIGNMENT.

**LEGEND:**

- ← DIRECTION OF SLOPE
- .XXXX. SAWCUT REQ'D (SAWCUTS TO BE AT NEAREST EXISTING JOINT)
- [Pattern] DETECTABLE WARNING FIELD
- [Pattern] GUTTER PAN SLOPE TRANSITION (6.25% TO 4.0%) (UNLESS OTHERWISE NOTED)
- [Pattern] COLORED AND STAMPED CONCRETE SIDEWALK
- (SW4) CONCRETE SIDEWALK 4-INCH
- (G) 6:1 GRADED FLARE
- (C) 10:1 CONCRETE FLARE
- (PED) CONCRETE CURB PEDESTRIAN
- (ADJ) ADJUSTING INLET
- (COV) INSTALL NEW INLET COVER (TYPE DENOTED IN MISC. QUANT.)
- (4.0%) GUTTER PAN SLOPE (SLOPE DENOTED)
- (NO. ELEV.) POINT NUMBER & ELEVATION

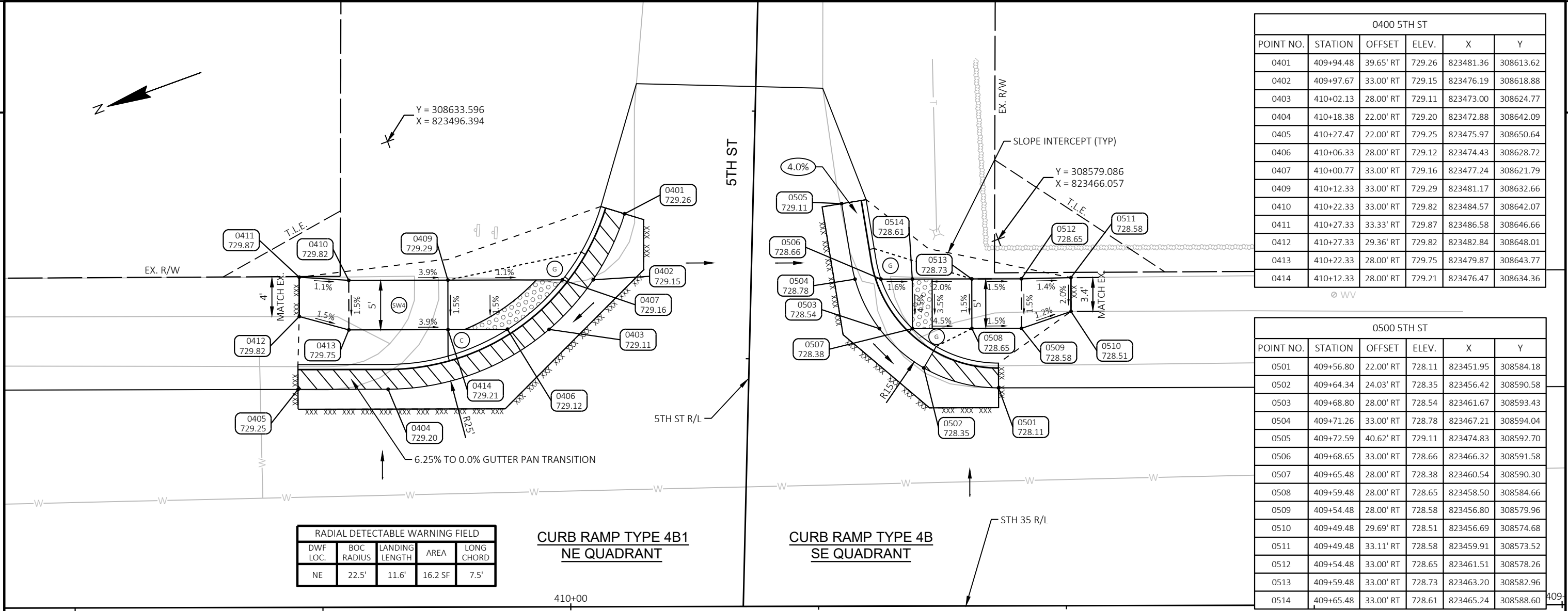


0200 6TH ST					
POINT NO.	STATION	OFFSET	ELEV.	X	Y
0201	413+76.62	40.15' RT	730.92	823601.61	308988.62
0202	413+78.95	33.51' RT	730.87	823595.02	308991.42
0203	413+81.46	28.51' RT	730.82	823590.04	308994.20
0204	413+87.17	23.36' RT	730.80	823584.92	308999.93
0205	413+97.47	20.50' RT	730.81	823582.13	309010.25
0206	413+84.73	28.50' RT	730.74	823590.05	308997.47
0207	413+81.43	33.50' RT	730.78	823595.03	308994.13
0208	413+84.73	33.50' RT	730.81	823595.05	308997.44
0209	413+91.73	33.50' RT	731.16	823595.09	309004.44
0210	413+96.73	33.50' RT	731.24	823595.12	309009.44
0211	414+01.73	33.24' RT	731.30	823594.89	309014.44
0212	414+01.73	29.96' RT	731.28	823591.61	309014.46
0213	413+96.73	28.50' RT	731.16	823590.12	309009.47
0214	413+91.73	28.50' RT	731.09	823590.09	309004.47

0300 6TH ST					
POINT NO.	STATION	OFFSET	ELEV.	X	Y
0301	413+34.22	25.09' RT	730.62	823581.49	308942.56
0302	413+40.70	28.03' RT	730.68	823585.68	308949.09
0303	413+43.94	31.60' RT	730.71	823589.79	308952.09
0304	413+46.10	36.29' RT	730.75	823594.80	308953.82
0305	413+46.57	45.57' RT	730.82	823604.06	308953.01
0306	413+43.91	36.66' RT	730.67	823594.78	308951.27
0307	413+41.40	32.05' RT	730.62	823589.77	308949.18
0308	413+38.89	32.52' RT	730.75	823589.76	308946.30
0309	413+34.38	32.81' RT	730.87	823589.09	308941.23
0310	413+33.87	36.63' RT	730.99	823592.72	308939.91
0311	413+38.89	37.60' RT	730.83	823594.75	308945.41
0312	413+42.09	36.99' RT	730.70	823594.77	308949.16

**LEGEND:**

- DIRECTION OF SLOPE
- .XXXX. SAWCUT REQ'D (SAWCUTS TO BE AT NEAREST EXISTING JOINT)
- [Hatched Box] DETECTABLE WARNING FIELD
- [Diagonal Lines Box] GUTTER PAN SLOPE TRANSITION (6.25% TO 4.0%) (UNLESS OTHERWISE NOTED)
- [Brick Pattern Box] COLORED AND STAMPED CONCRETE SIDEWALK
- (SW4) CONCRETE SIDEWALK 4-INCH
- (G) 6:1 GRADED FLARE
- (C) 10:1 CONCRETE FLARE
- (PED) CONCRETE CURB PEDESTRIAN
- (ADJ) ADJUSTING INLET
- (COV) INSTALL NEW INLET COVER (TYPE DENOTED IN MISC. QUANT.)
- (4.0%) GUTTER PAN SLOPE (SLOPE DENOTED)
- (NO. ELEV.) POINT NUMBER & ELEVATION



0400 5TH ST					
POINT NO.	STATION	OFFSET	ELEV.	X	Y
0401	409+94.48	39.65' RT	729.26	823481.36	308613.62
0402	409+97.67	33.00' RT	729.15	823476.19	308618.88
0403	410+02.13	28.00' RT	729.11	823473.00	308624.77
0404	410+18.38	22.00' RT	729.20	823472.88	308642.09
0405	410+27.47	22.00' RT	729.25	823475.97	308650.64
0406	410+06.33	28.00' RT	729.12	823474.43	308628.72
0407	410+00.77	33.00' RT	729.16	823477.24	308621.79
0409	410+12.33	33.00' RT	729.29	823481.17	308632.66
0410	410+22.33	33.00' RT	729.82	823484.57	308642.07
0411	410+27.33	33.33' RT	729.87	823486.58	308646.66
0412	410+27.33	29.36' RT	729.82	823482.84	308648.01
0413	410+22.33	28.00' RT	729.75	823479.87	308643.77
0414	410+12.33	28.00' RT	729.21	823476.47	308634.36

0500 5TH ST					
POINT NO.	STATION	OFFSET	ELEV.	X	Y
0501	409+56.80	22.00' RT	728.11	823451.95	308584.18
0502	409+64.34	24.03' RT	728.35	823456.42	308590.58
0503	409+68.80	28.00' RT	728.54	823461.67	308593.43
0504	409+71.26	33.00' RT	728.78	823467.21	308594.04
0505	409+72.59	40.62' RT	729.11	823474.83	308592.70
0506	409+68.65	33.00' RT	728.66	823466.32	308591.58
0507	409+65.48	28.00' RT	728.38	823460.54	308590.30
0508	409+59.48	28.00' RT	728.65	823458.50	308584.66
0509	409+54.48	28.00' RT	728.58	823456.80	308579.96
0510	409+49.48	29.69' RT	728.51	823456.69	308574.68
0511	409+49.48	33.11' RT	728.58	823459.91	308573.52
0512	409+54.48	33.00' RT	728.65	823461.51	308578.26
0513	409+59.48	33.00' RT	728.73	823463.20	308582.96
0514	409+65.48	33.00' RT	728.61	823465.24	308588.60

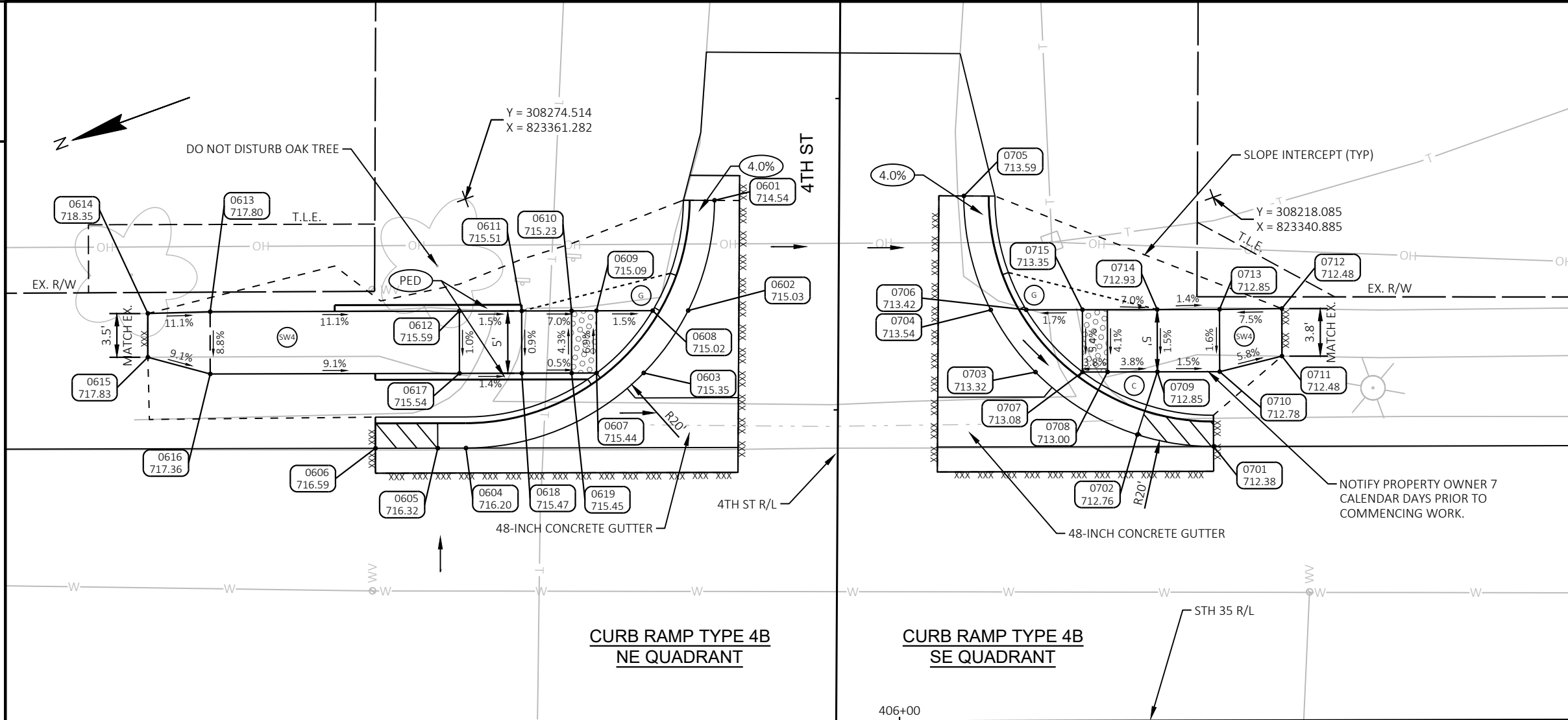
RADIAL DETECTABLE WARNING FIELD				
DWF LOC.	BOC RADIUS	LANDING LENGTH	AREA	LONG CHORD
NE	22.5'	11.6'	16.2 SF	7.5'

**CURB RAMP TYPE 4B1  
NE QUADRANT**

**CURB RAMP TYPE 4B  
SE QUADRANT**

**LEGEND:**

- DIRECTION OF SLOPE
- .XXXX. SAWCUT REQ'D (SAWCUTS TO BE AT NEAREST EXISTING JOINT)
- [Hatched Box] DETECTABLE WARNING FIELD
- [Diagonal Lines] GUTTER PAN SLOPE TRANSITION (6.25% TO 4.0%) (UNLESS OTHERWISE NOTED)
- [Brick Pattern] COLORED AND STAMPED CONCRETE SIDEWALK
- (SW4) CONCRETE SIDEWALK 4-INCH
- (G) 6:1 GRADED FLARE
- (C) 10:1 CONCRETE FLARE
- (PED) CONCRETE CURB PEDESTRIAN
- (ADJ) ADJUSTING INLET
- (COV) INSTALL NEW INLET COVER (TYPE DENOTED IN MISC. QUANT.)
- (4.0%) GUTTER PAN SLOPE (SLOPE DENOTED)
- (NO. ELEV.) POINT NUMBER & ELEVATION

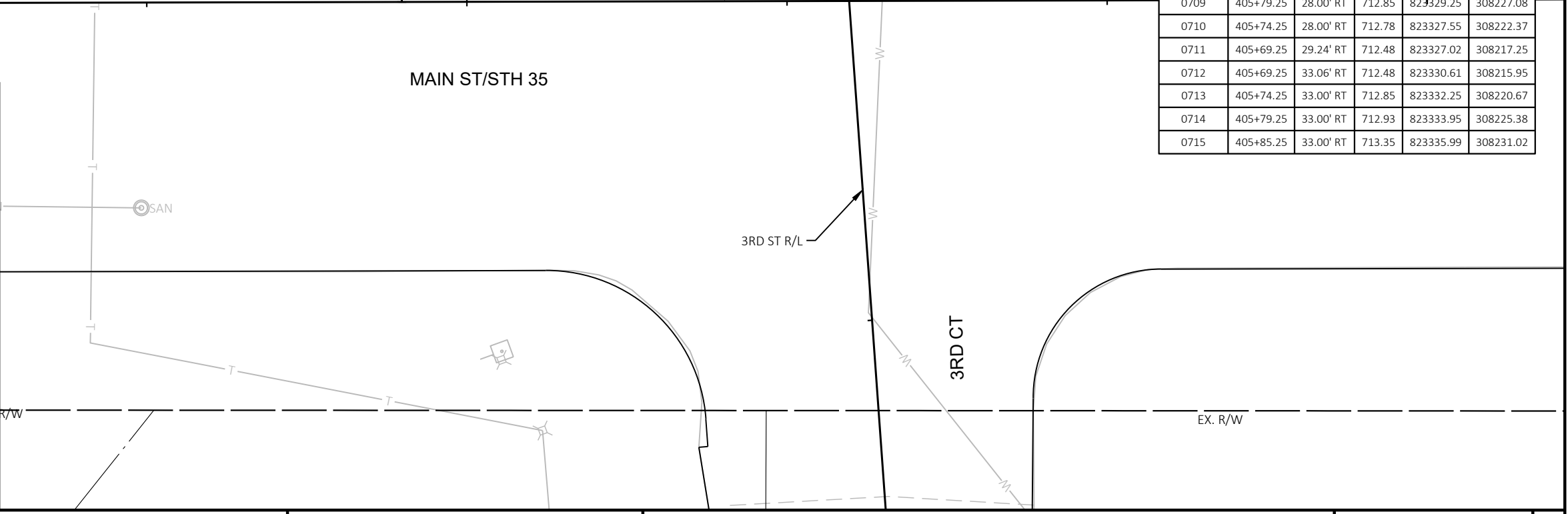


0600 4TH ST					
POINT NO.	STATION	OFFSET	ELEV.	X	Y
0601	406+14.75	41.84' RT	714.54	823354.34	308255.76
0602	406+16.89	33.00' RT	715.03	823346.75	308260.78
0603	406+20.47	28.00' RT	715.35	823343.26	308265.84
0604	406+34.75	22.00' RT	716.20	823342.47	308281.31
0605	406+37.01	22.00' RT	716.32	823343.24	308283.44
0606	406+42.01	22.00' RT	716.59	823344.94	308288.14
0607	406+24.25	28.00' RT	715.44	823344.55	308269.40
0608	406+19.74	33.00' RT	715.02	823347.72	308263.46
0609	406+24.25	33.00' RT	715.09	823349.25	308267.70
0610	406+26.25	33.00' RT	715.23	823349.93	308269.58
0611	406+30.25	33.00' RT	715.51	823351.29	308273.34
0612	406+35.25	33.00' RT	715.59	823352.99	308278.04
0613	406+55.25	33.00' RT	717.80	823359.79	308296.85
0614	406+60.25	32.86' RT	718.35	823361.36	308301.60
0615	406+60.25	29.35' RT	717.83	823358.06	308302.79
0616	406+55.25	28.00' RT	717.36	823355.08	308298.55
0617	406+35.25	28.00' RT	715.54	823348.29	308279.74
0618	406+30.25	28.00' RT	715.47	823346.59	308275.04

0700 4TH ST					
POINT NO.	STATION	OFFSET	ELEV.	X	Y
0701	405+74.75	22.00' RT	712.38	823322.08	308224.88
0702	405+80.87	22.96' RT	712.76	823325.06	308230.31
0703	405+89.03	28.00' RT	713.32	823332.57	308236.28
0704	405+92.61	33.00' RT	713.54	823338.49	308237.94
0705	405+94.75	42.16' RT	713.59	823347.83	308236.84
0706	405+89.76	33.00' RT	713.42	823337.52	308235.26
0707	405+85.25	28.00' RT	713.08	823331.29	308232.72
0708	405+83.25	28.00' RT	713.00	823330.61	308230.84
0709	405+79.25	28.00' RT	712.85	823329.25	308227.08
0710	405+74.25	28.00' RT	712.78	823327.55	308222.37
0711	405+69.25	29.24' RT	712.48	823327.02	308217.25
0712	405+69.25	33.06' RT	712.48	823330.61	308215.95
0713	405+74.25	33.00' RT	712.85	823332.25	308220.67
0714	405+79.25	33.00' RT	712.93	823333.95	308225.38
0715	405+85.25	33.00' RT	713.35	823335.99	308231.02

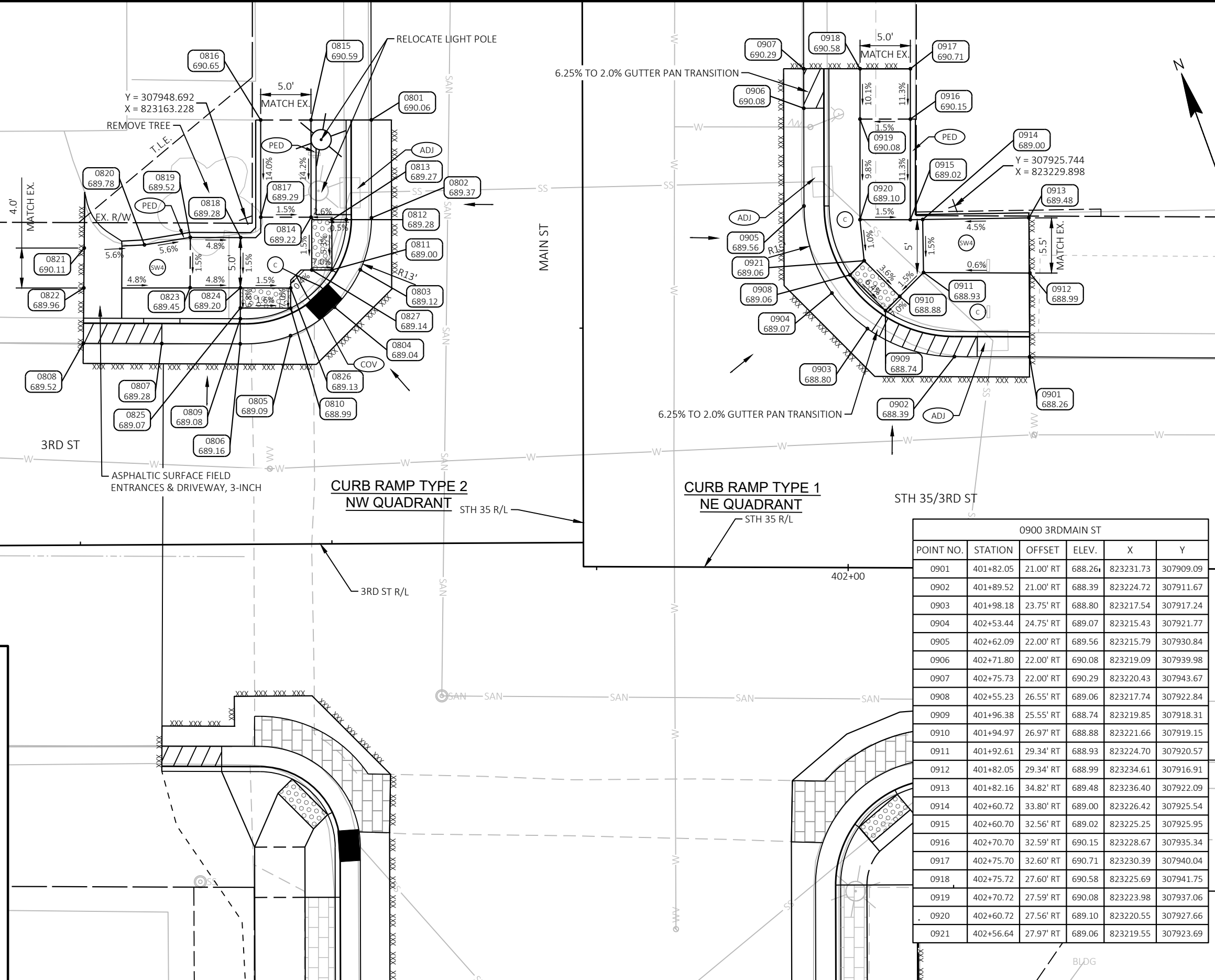
**LEGEND:**

- DIRECTION OF SLOPE
- .XXXX. SAWCUT REQ'D (SAWCUTS TO BE AT NEAREST EXISTING JOINT)
- [Pattern] DETECTABLE WARNING FIELD
- [Pattern] GUTTER PAN SLOPE TRANSITION (6.25% TO 4.0%) (UNLESS OTHERWISE NOTED)
- [Pattern] COLORED AND STAMPED CONCRETE SIDEWALK
- (SW4) CONCRETE SIDEWALK 4-INCH
- (G) 6:1 GRADED FLARE
- (C) 10:1 CONCRETE FLARE
- (PED) CONCRETE CURB PEDESTRIAN
- (ADJ) ADJUSTING INLET
- (COV) INSTALL NEW INLET COVER (TYPE DENOTED IN MISC. QUANT.)
- (4.0%) GUTTER PAN SLOPE (SLOPE DENOTED)
- (NO. ELEV.) POINT NUMBER & ELEVATION



0800 3RDMAIN ST					
POINT NO.	STATION	OFFSET	ELEV.	X	Y
0801	402+70.74	21.00' LT	690.06	823178.29	307953.59
0802	402+61.01	21.00' LT	689.37	823174.98	307944.44
0803	402+55.84	22.12' LT	689.12	823172.18	307939.96
0804	402+52.14	24.69' LT	689.04	823168.50	307937.36
0805	402+49.32	29.07' LT	689.09	823163.42	307936.19
0806	402+48.51	34.07' LT	689.16	823158.44	307937.13
0807	402+48.56	41.87' LT	689.28	823151.13	307939.83
0808	402+48.60	49.67' LT	689.52	823143.80	307942.52
0809	402+51.01	34.06' LT	689.08	823159.30	307939.48
0810	402+52.05	29.05' LT	688.99	823164.37	307938.76
0811	402+55.84	24.94' LT	689.00	823169.52	307940.92
0812	402+60.84	23.50' LT	689.28	823172.57	307945.13
0813	402+60.84	24.94' LT	689.27	823171.22	307945.62
0814	402+61.09	27.00' LT	689.22	823169.36	307946.56
0815	402+70.74	26.99' LT	690.59	823172.66	307955.63
0816	402+70.75	31.99' LT	690.65	823167.96	307957.34
0817	402+61.10	32.00' LT	689.29	823164.67	307948.27
0818	402+59.08	34.01' LT	689.28	823162.09	307947.05
0819	402+59.11	39.01' LT	689.52	823157.40	307948.78
0820	402+58.37	43.57' LT	689.78	823152.86	307949.64
0821	402+58.07	49.56' LT	690.11	823147.13	307951.39
0822	402+54.11	49.61' LT	689.96	823145.74	307947.68
0823	402+54.11	39.04' LT	689.45	823155.67	307944.09
0824	402+54.08	34.04' LT	689.20	823160.36	307942.36
0825	402+52.08	34.05' LT	689.07	823159.67	307940.48
0826	402+54.05	29.04' LT	689.13	823165.06	307940.63
0827	402+55.84	26.94' LT	689.14	823167.64	307941.60

0900 3RDMAIN ST					
POINT NO.	STATION	OFFSET	ELEV.	X	Y
0901	401+82.05	21.00' RT	688.26	823231.73	307909.09
0902	401+89.52	21.00' RT	688.39	823224.72	307911.67
0903	401+98.18	23.75' RT	688.80	823217.54	307917.24
0904	402+53.44	24.75' RT	689.07	823215.43	307921.77
0905	402+62.09	22.00' RT	689.56	823215.79	307930.84
0906	402+71.80	22.00' RT	690.08	823219.09	307939.98
0907	402+75.73	22.00' RT	690.29	823220.43	307943.67
0908	402+55.23	26.55' RT	689.06	823217.74	307922.84
0909	401+96.38	25.55' RT	688.74	823219.85	307918.31
0910	401+94.97	26.97' RT	688.88	823221.66	307919.15
0911	401+92.61	29.34' RT	688.93	823224.70	307920.57
0912	401+82.05	29.34' RT	688.99	823234.61	307916.91
0913	401+82.16	34.82' RT	689.48	823236.40	307922.09
0914	402+60.72	33.80' RT	689.00	823226.42	307925.54
0915	402+60.70	32.56' RT	689.02	823225.25	307925.95
0916	402+70.70	32.59' RT	690.15	823228.67	307935.34
0917	402+75.70	32.60' RT	690.71	823230.39	307940.04
0918	402+75.72	27.60' RT	690.58	823225.69	307941.75
0919	402+70.72	27.59' RT	690.08	823223.98	307937.06
0920	402+60.72	27.56' RT	689.10	823220.55	307927.66
0921	402+56.64	27.97' RT	689.06	823219.55	307923.69



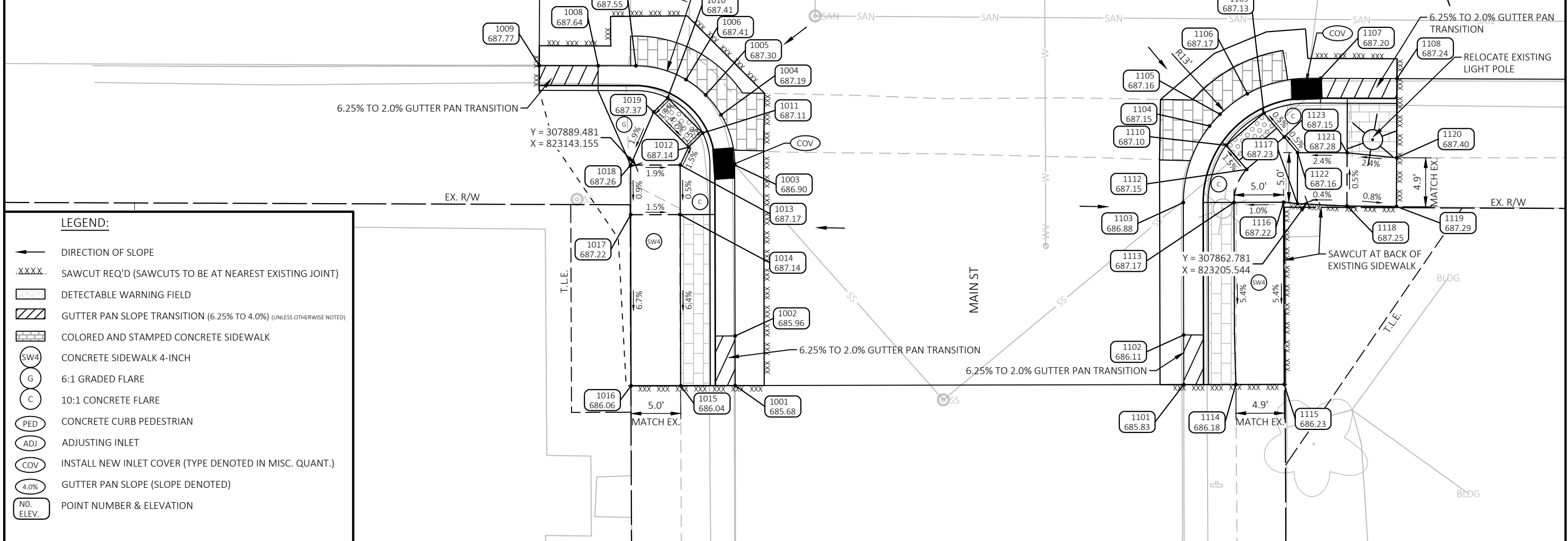
**LEGEND:**

- DIRECTION OF SLOPE
- .XXXX. SAWCUT REQ'D (SAWCUTS TO BE AT NEAREST EXISTING JOINT)
- [Pattern] DETECTABLE WARNING FIELD
- [Pattern] GUTTER PAN SLOPE TRANSITION (6.25% TO 4.0%) (UNLESS OTHERWISE NOTED)
- [Pattern] COLORED AND STAMPED CONCRETE SIDEWALK
- (SW4) CONCRETE SIDEWALK 4-INCH
- (G) 6:1 GRADED FLARE
- (C) 10:1 CONCRETE FLARE
- (PED) CONCRETE CURB PEDESTRIAN
- (ADJ) ADJUSTING INLET
- (COV) INSTALL NEW INLET COVER (TYPE DENOTED IN MISC. QUANT.)
- (4.0%) GUTTER PAN SLOPE (SLOPE DENOTED)
- (NO. ELEV.) POINT NUMBER & ELEVATION

1000 3RDMAIN ST					
POINT NO.	STATION	OFFSET	ELEV.	X	Y
1001	402+26.31	54.78' LT	685.68	823145.00	307865.16
1002	402+26.31	50.25' LT	685.96	823146.70	307869.86
1003	402+26.31	35.61' LT	686.90	823152.56	307886.08
1004	402+26.31	32.80' LT	687.19	823152.99	307891.30
1005	402+26.31	32.62' LT	687.30	823152.22	307893.70
1006	402+26.31	33.26' LT	687.41	823150.88	307895.83
1007	402+26.31	36.79' LT	687.55	823146.61	307898.87
1008	402+26.31	40.15' LT	687.64	823143.04	307900.18
1009	402+26.31	45.54' LT	687.77	823137.46	307902.23
1010	402+26.31	35.83' LT	687.41	823148.51	307894.73
1011	402+26.31	35.41' LT	687.11	823150.62	307890.20
1012	402+26.31	37.41' LT	687.14	823148.81	307889.35
1013	402+26.31	39.29' LT	687.17	823147.39	307887.95
1014	402+26.31	43.00' LT	687.14	823145.64	307883.26
1015	402+26.31	57.23' LT	686.04	823139.82	307867.03
1016	402+26.31	59.82' LT	686.06	823135.12	307868.73
1017	402+26.31	46.36' LT	687.22	823140.99	307884.95
1018	402+26.31	42.96' LT	687.26	823142.68	307889.65
1019	402+26.31	37.81' LT	687.37	823146.70	307893.89

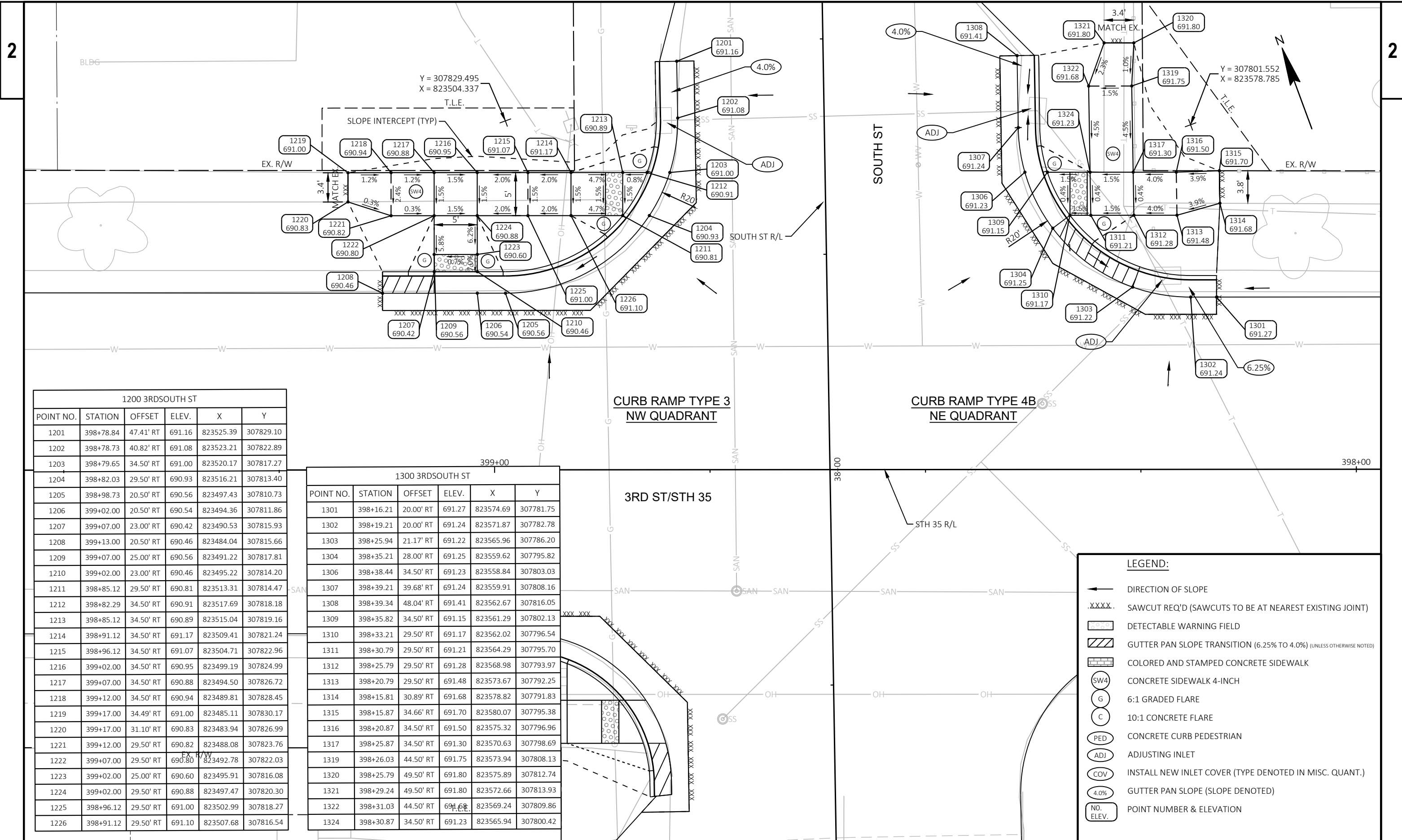
1100 3RDMAIN ST					
POINT NO.	STATION	OFFSET	ELEV.	X	Y
1101	402+03.01	49.92' LT	685.83	823187.55	307849.78
1102	402+03.04	44.92' LT	686.11	823189.25	307854.48
1103	402+03.12	31.57' LT	686.88	823193.79	307867.03
1104	402+00.50	23.84' LT	687.15	823198.92	307873.38
1105	401+99.46	22.66' LT	687.16	823200.31	307874.13
1106	401+96.71	20.58' LT	687.17	823203.60	307875.13
1107	401+89.33	19.00' LT	687.20	823211.08	307874.06
1108	401+81.62	19.00' LT	687.24	823218.31	307871.40
1109	401+95.04	22.53' LT	687.13	823204.50	307872.73
1110	401+98.83	25.79' LT	687.10	823199.82	307870.98
1112	401+96.75	28.21' LT	687.15	823200.93	307867.99

1100 3RDMAIN ST					
POINT NO.	STATION	OFFSET	ELEV.	X	Y
1113	401+98.02	31.54' LT	687.17	823198.59	307865.30
1114	401+97.75	49.89' LT	686.18	823192.50	307847.99
1115	401+92.88	49.86' LT	686.23	823197.08	307846.33
1116	401+93.02	31.50' LT	687.22	823203.30	307863.61
1117	401+91.62	31.63' LT	687.23	823204.56	307863.00
1118	401+86.62	31.86' LT	687.25	823209.17	307861.07
1119	401+81.62	31.89' LT	687.29	823213.85	307859.31
1120	401+81.62	26.98' LT	687.40	823215.55	307863.92
1121	401+86.62	26.50' LT	687.28	823211.02	307866.09
1122	401+91.62	26.50' LT	687.16	823206.33	307867.82
1123	401+92.97	24.93' LT	687.15	823205.61	307869.76



**LEGEND:**

- DIRECTION OF SLOPE
- .XXXX. SAWCUT REQ'D (SAWCUTS TO BE AT NEAREST EXISTING JOINT)
- [Pattern] DETECTABLE WARNING FIELD
- [Pattern] GUTTER PAN SLOPE TRANSITION (6.25% TO 4.0%) (UNLESS OTHERWISE NOTED)
- [Pattern] COLORED AND STAMPED CONCRETE SIDEWALK
- (SW4) CONCRETE SIDEWALK 4-INCH
- (G) 6:1 GRADED FLARE
- (C) 10:1 CONCRETE FLARE
- (PED) CONCRETE CURB PEDESTRIAN
- (ADJ) ADJUSTING INLET
- (COV) INSTALL NEW INLET COVER (TYPE DENOTED IN MISC. QUANT.)
- (4.0%) GUTTER PAN SLOPE (SLOPE DENOTED)
- (NO. ELEV.) POINT NUMBER & ELEVATION



1200 3RDSOUTH ST					
POINT NO.	STATION	OFFSET	ELEV.	X	Y
1201	398+78.84	47.41' RT	691.16	823525.39	307829.10
1202	398+78.73	40.82' RT	691.08	823523.21	307822.89
1203	398+79.65	34.50' RT	691.00	823520.17	307817.27
1204	398+82.03	29.50' RT	690.93	823516.21	307813.40
1205	398+98.73	20.50' RT	690.56	823497.43	307810.73
1206	399+02.00	20.50' RT	690.54	823494.36	307811.86
1207	399+07.00	23.00' RT	690.42	823490.53	307815.93
1208	399+13.00	20.50' RT	690.46	823484.04	307815.66
1209	399+07.00	25.00' RT	690.56	823491.22	307817.81
1210	399+02.00	23.00' RT	690.46	823495.22	307814.20
1211	398+85.12	29.50' RT	690.81	823513.31	307814.47
1212	398+82.29	34.50' RT	690.91	823517.69	307818.18
1213	398+85.12	34.50' RT	690.89	823515.04	307819.16
1214	398+91.12	34.50' RT	691.17	823509.41	307821.24
1215	398+96.12	34.50' RT	691.07	823504.71	307822.96
1216	399+02.00	34.50' RT	690.95	823499.19	307824.99
1217	399+07.00	34.50' RT	690.88	823494.50	307826.72
1218	399+12.00	34.50' RT	690.94	823489.81	307828.45
1219	399+17.00	34.49' RT	691.00	823485.11	307830.17
1220	399+17.00	31.10' RT	690.83	823483.94	307826.99
1221	399+12.00	29.50' RT	690.82	823488.08	307823.76
1222	399+07.00	29.50' RT	690.80	823492.78	307822.03
1223	399+02.00	25.00' RT	690.60	823495.91	307816.08
1224	399+02.00	29.50' RT	690.88	823497.47	307820.30
1225	398+96.12	29.50' RT	691.00	823502.99	307818.27
1226	398+91.12	29.50' RT	691.10	823507.68	307816.54

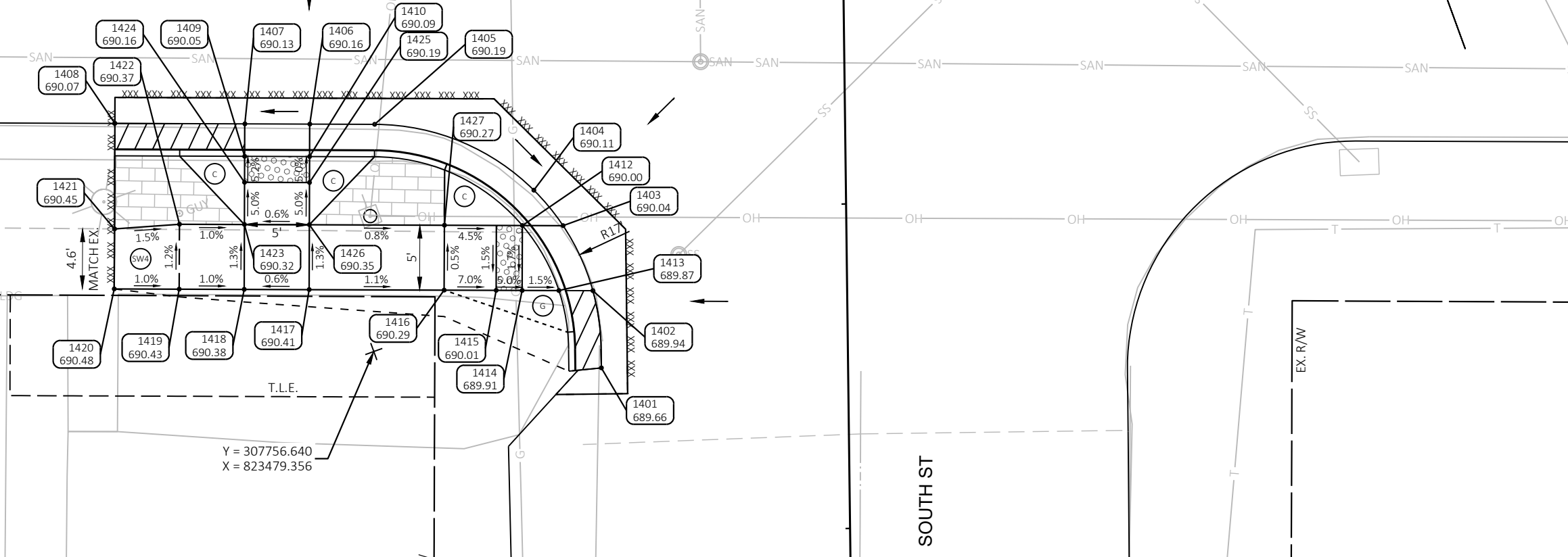
1300 3RDSOUTH ST					
POINT NO.	STATION	OFFSET	ELEV.	X	Y
1301	398+16.21	20.00' RT	691.27	823574.69	307781.75
1302	398+19.21	20.00' RT	691.24	823571.87	307782.78
1303	398+25.94	21.17' RT	691.22	823565.96	307786.20
1304	398+35.21	28.00' RT	691.25	823559.62	307795.82
1306	398+38.44	34.50' RT	691.23	823558.84	307803.03
1307	398+39.21	39.68' RT	691.24	823559.91	307808.16
1308	398+39.34	48.04' RT	691.41	823562.67	307816.05
1309	398+35.82	34.50' RT	691.15	823561.29	307802.13
1310	398+33.21	29.50' RT	691.17	823562.02	307796.54
1311	398+30.79	29.50' RT	691.21	823564.29	307795.70
1312	398+25.79	29.50' RT	691.28	823568.98	307793.97
1313	398+20.79	29.50' RT	691.48	823573.67	307792.25
1314	398+15.81	30.89' RT	691.68	823578.82	307791.83
1315	398+15.87	34.66' RT	691.70	823580.07	307795.38
1316	398+20.87	34.50' RT	691.50	823575.32	307796.96
1317	398+25.87	34.50' RT	691.30	823570.63	307798.69
1319	398+26.03	44.50' RT	691.75	823573.94	307808.13
1320	398+25.79	49.50' RT	691.80	823575.89	307812.74
1321	398+29.24	49.50' RT	691.80	823572.66	307813.93
1322	398+31.03	44.50' RT	691.68	823569.24	307809.86
1324	398+30.87	34.50' RT	691.23	823565.94	307800.42

**LEGEND:**

- DIRECTION OF SLOPE
- .XXXX. SAWCUT REQ'D (SAWCUTS TO BE AT NEAREST EXISTING JOINT)
- DETECTABLE WARNING FIELD
- GUTTER PAN SLOPE TRANSITION (6.25% TO 4.0%) (UNLESS OTHERWISE NOTED)
- COLORED AND STAMPED CONCRETE SIDEWALK
- CONCRETE SIDEWALK 4-INCH
- 6:1 GRADED FLARE
- 10:1 CONCRETE FLARE
- CONCRETE CURB PEDESTRIAN
- ADJUSTING INLET
- INSTALL NEW INLET COVER (TYPE DENOTED IN MISC. QUANT.)
- GUTTER PAN SLOPE (SLOPE DENOTED)
- POINT NUMBER & ELEVATION



3RD ST/STH 35  
CURB RAMP TYPE 3  
SW QUADRANT



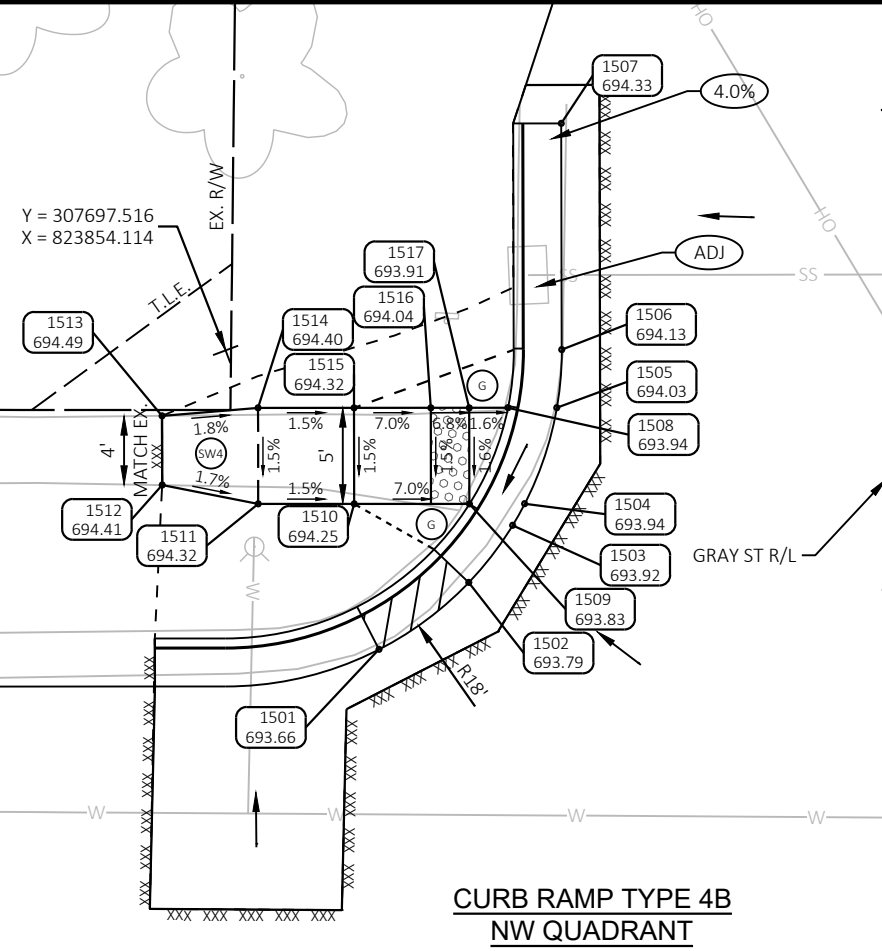
1400 3RDSOUTH ST					
POINT NO.	STATION	OFFSET	ELEV.	X	Y
1401	398+79.48	37.70' LT	689.66	823495.38	307749.46
1402	398+80.16	31.75' LT	689.94	823496.80	307755.28
1403	398+82.47	26.75' LT	690.04	823496.36	307760.77
1404	398+84.72	24.03' LT	690.11	823495.18	307764.10
1405	398+97.00	19.00' LT	690.19	823485.40	307773.06
1406	399+01.99	19.00' LT	690.16	823480.72	307774.79
1407	399+06.99	19.00' LT	690.13	823476.03	307776.51
1408	399+17.00	19.00' LT	690.07	823466.63	307779.97
1409	399+07.00	21.50' LT	690.05	823475.15	307774.17
1410	399+02.00	21.50' LT	690.09	823479.85	307772.44
1412	398+85.60	26.75' LT	690.00	823493.42	307761.85
1413	398+82.77	31.75' LT	689.87	823494.35	307756.18
1414	398+85.60	31.75' LT	689.91	823491.69	307757.16
1415	398+87.60	31.75' LT	690.01	823489.82	307757.85
1416	398+91.60	31.75' LT	690.29	823486.06	307759.23
1417	399+02.00	31.75' LT	690.41	823476.30	307762.82
1418	399+07.00	31.75' LT	690.38	823471.61	307764.55
1419	399+12.00	31.75' LT	690.43	823466.92	307766.28
1420	399+17.00	31.75' LT	690.48	823462.23	307768.01
1421	399+17.00	27.13' LT	690.45	823463.83	307772.34
1422	399+12.00	26.75' LT	690.37	823468.65	307770.97
1423	399+07.00	26.75' LT	690.32	823473.34	307769.24
1424	399+07.00	23.50' LT	690.16	823474.46	307772.29
1425	399+02.00	23.50' LT	690.19	823479.16	307770.57
1426	399+02.00	26.75' LT	690.35	823478.03	307767.52
1427	398+91.60	26.75' LT	690.27	823487.79	307763.92

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X = 823479.356

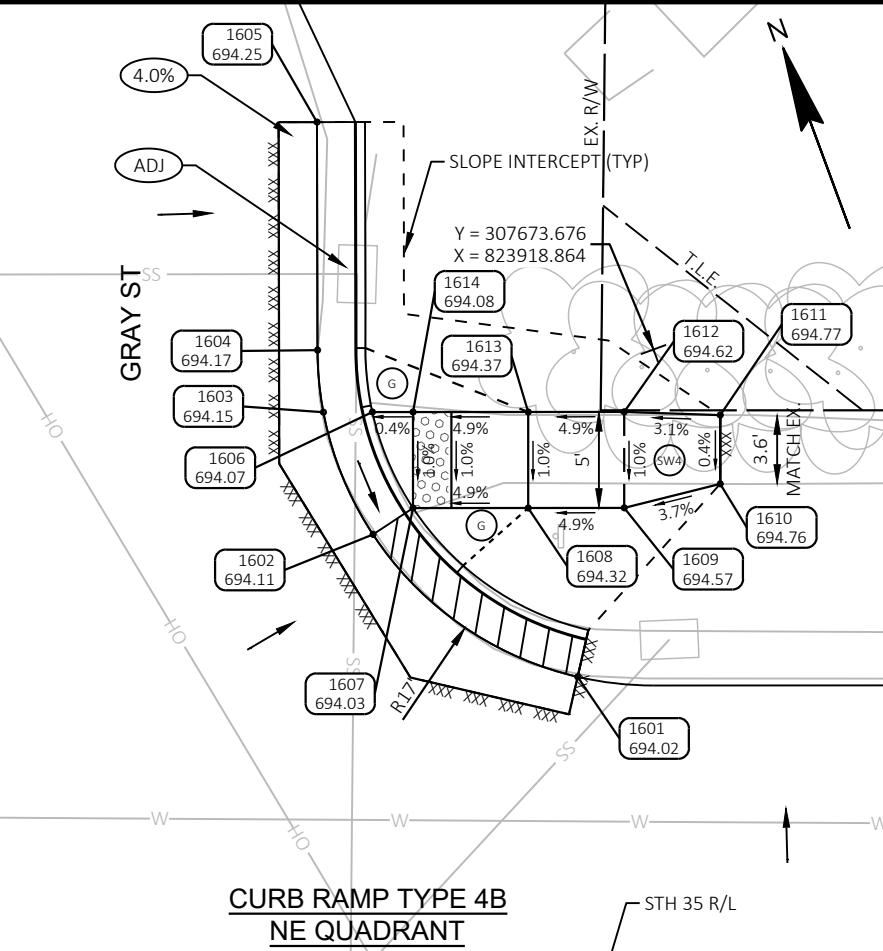
**LEGEND:**

- ← DIRECTION OF SLOPE
- .XXXX. SAWCUT REQ'D (SAWCUTS TO BE AT NEAREST EXISTING JOINT)
- [Pattern] DETECTABLE WARNING FIELD
- [Pattern] GUTTER PAN SLOPE TRANSITION (6.25% TO 4.0%) (UNLESS OTHERWISE NOTED)
- [Pattern] COLORED AND STAMPED CONCRETE SIDEWALK
- (SW4) CONCRETE SIDEWALK 4-INCH
- (G) 6:1 GRADED FLARE
- (C) 10:1 CONCRETE FLARE
- (PED) CONCRETE CURB PEDESTRIAN
- (ADJ) ADJUSTING INLET
- (COV) INSTALL NEW INLET COVER (TYPE DENOTED IN MISC. QUANT.)
- (4.0%) GUTTER PAN SLOPE (SLOPE DENOTED)
- (NO. ELEV.) POINT NUMBER & ELEVATION

1500 3RDGRAY ST					
POINT NO.	STATION	OFFSET	ELEV.	X	Y
1501	395+16.92	21.92' RT	693.66	823856.21	307680.14
1502	395+12.24	25.41' RT	693.79	823861.81	307681.80
1503	395+09.96	28.38' RT	693.92	823864.98	307683.80
1504	395+09.33	29.50' RT	693.94	823865.96	307684.63
1505	395+07.65	34.50' RT	694.03	823869.26	307688.74
1506	395+07.39	37.52' RT	694.13	823870.54	307691.49
1507	395+07.41	49.31' RT	694.33	823874.60	307702.56
1508	395+10.20	34.50' RT	693.94	823866.87	307689.62
1509	395+12.20	29.50' RT	693.83	823863.26	307685.62
1510	395+18.20	29.50' RT	694.25	823857.63	307687.70
1511	395+23.20	29.50' RT	694.32	823852.93	307689.43
1512	395+28.20	30.49' RT	694.41	823848.58	307692.08
1513	395+28.20	34.09' RT	694.49	823849.83	307695.46
1514	395+23.20	34.50' RT	694.40	823854.66	307694.12
1515	395+18.20	34.50' RT	694.32	823859.35	307692.39
1516	395+14.20	34.50' RT	694.04	823863.11	307691.01
1517	395+12.20	34.50' RT	693.91	823864.98	307690.32

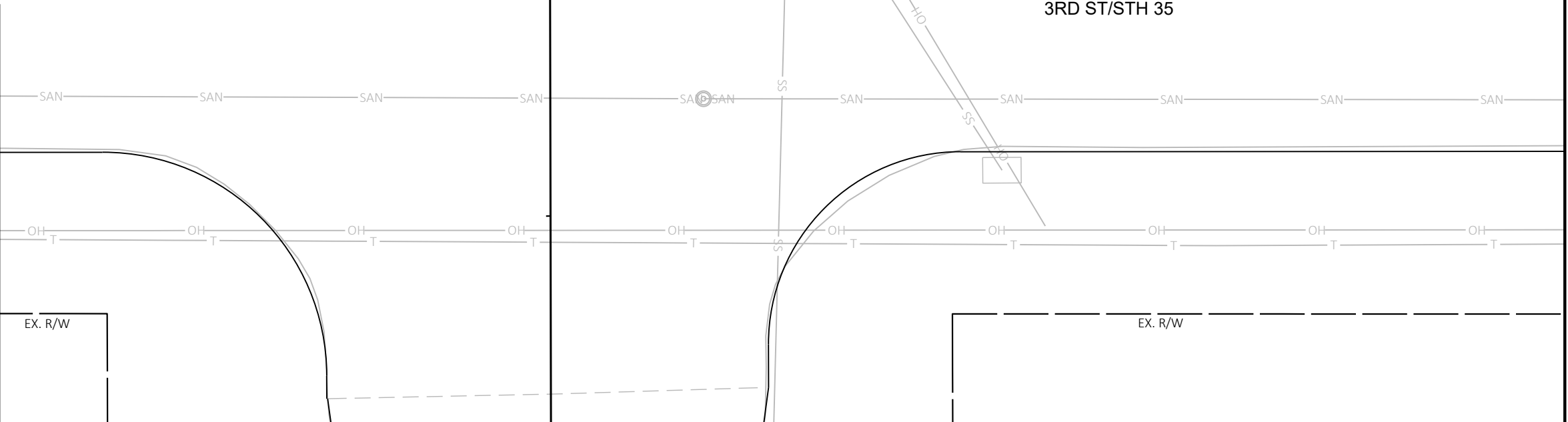


1600 3RDGRAY ST					
POINT NO.	STATION	OFFSET	ELEV.	X	Y
1601	394+59.86	20.45' RT	694.02	823909.26	307659.05
1602	394+70.51	27.87' RT	694.11	823901.82	307669.69
1603	394+73.09	34.25' RT	694.15	823901.60	307676.57
1604	394+73.39	37.48' RT	694.17	823902.43	307679.70
1605	394+73.41	49.36' RT	694.25	823906.52	307690.85
1606	394+70.54	34.25' RT	694.07	823904.00	307675.69
1607	394+68.42	29.25' RT	694.03	823904.26	307670.26
1608	394+62.42	29.25' RT	694.32	823909.89	307668.19
1609	394+57.42	29.25' RT	694.57	823914.58	307666.46
1610	394+52.42	30.50' RT	694.76	823919.70	307665.91
1611	394+52.42	34.08' RT	694.77	823920.94	307669.27
1612	394+57.42	34.25' RT	694.62	823916.31	307671.15
1613	394+62.42	34.25' RT	694.37	823911.62	307672.88
1614	394+68.42	34.25' RT	694.08	823905.99	307674.95



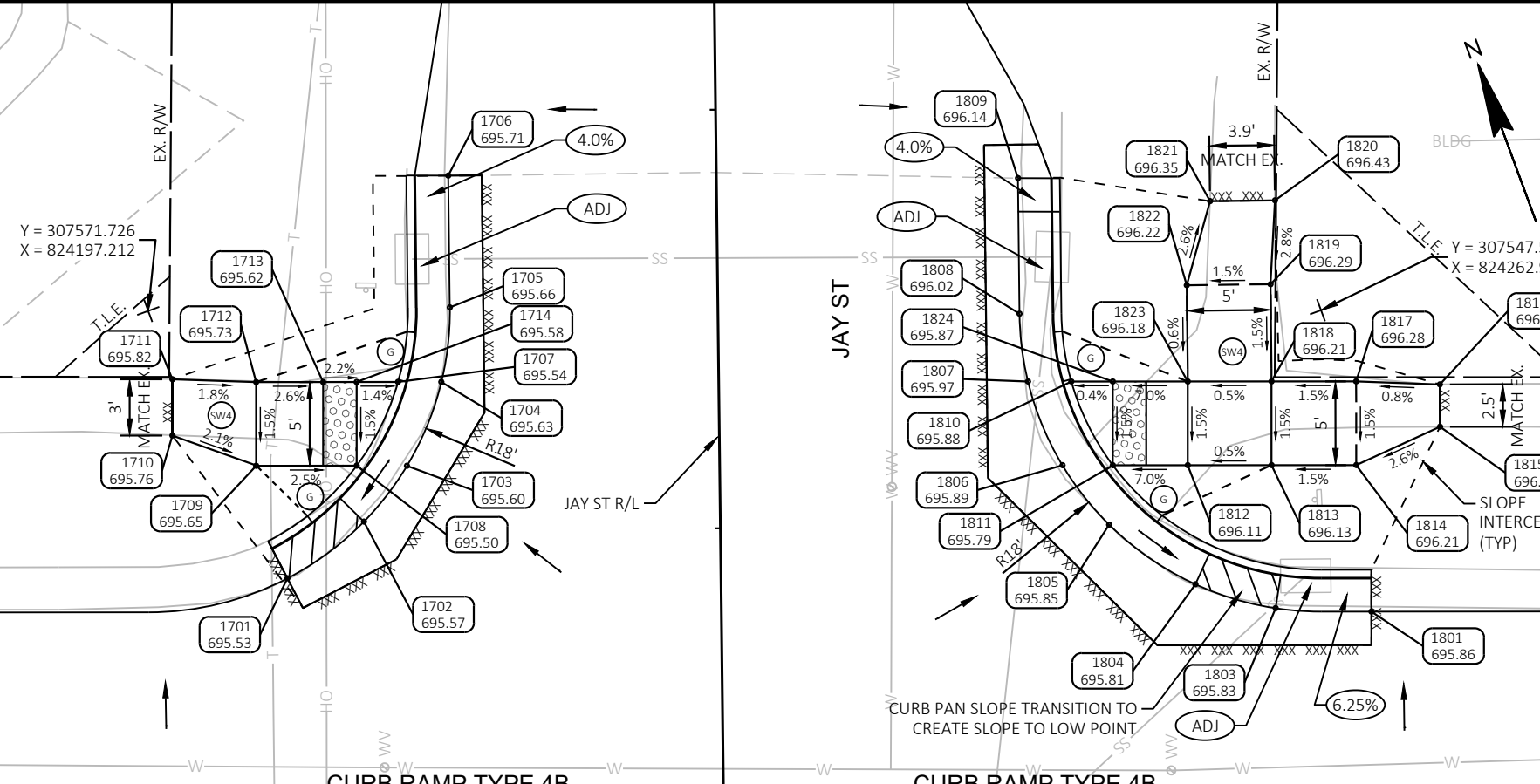
**LEGEND:**

- DIRECTION OF SLOPE
- .XXXX. SAWCUT REQ'D (SAWCUTS TO BE AT NEAREST EXISTING JOINT)
- DETECTABLE WARNING FIELD
- GUTTER PAN SLOPE TRANSITION (6.25% TO 4.0%) (UNLESS OTHERWISE NOTED)
- COLORED AND STAMPED CONCRETE SIDEWALK
- CONCRETE SIDEWALK 4-INCH
- 6:1 GRADED FLARE
- 10:1 CONCRETE FLARE
- CONCRETE CURB PEDESTRIAN
- ADJUSTING INLET
- INSTALL NEW INLET COVER (TYPE DENOTED IN MISC. QUANT.)
- GUTTER PAN SLOPE (SLOPE DENOTED)
- POINT NUMBER & ELEVATION



1700 3RDJAY ST					
POINT NO.	STATION	OFFSET	ELEV.	X	Y
1701	391+51.16	22.03' RT	695.53	824199.48	307553.87
1702	391+46.59	25.41' RT	695.57	824204.94	307555.47
1703	391+44.03	28.74' RT	695.60	824208.50	307557.70
1704	391+41.97	33.75' RT	695.63	824212.16	307561.69
1705	391+41.46	38.20' RT	695.66	824214.17	307565.69
1706	391+41.55	46.07' RT	695.71	824216.81	307573.11
1707	391+44.56	33.75' RT	695.54	824209.73	307562.59
1708	391+47.03	28.75' RT	695.50	824205.69	307558.75
1709	391+53.03	28.75' RT	695.65	824200.06	307560.82
1710	391+58.03	30.56' RT	695.76	824195.99	307564.25
1711	391+58.03	33.92' RT	695.82	824197.15	307567.40
1712	391+53.03	33.75' RT	695.73	824201.78	307565.51
1713	391+49.03	33.75' RT	695.62	824205.54	307564.13
1714	391+47.03	33.75' RT	695.58	824207.41	307563.44

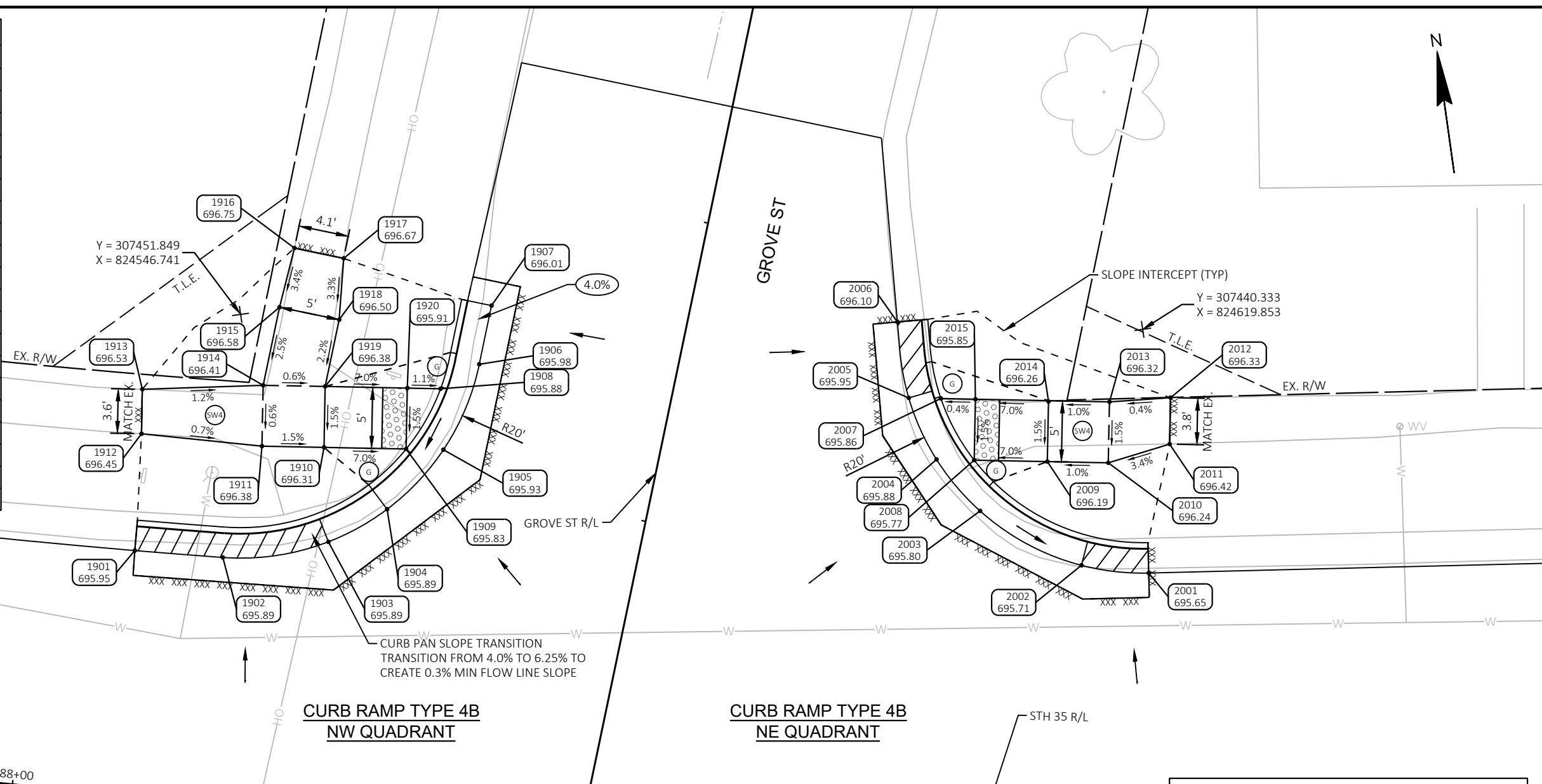
1800 3RDJAY ST					
POINT NO.	STATION	OFFSET	ELEV.	X	Y
1801	390+86.46	20.00' RT	695.86	824259.50	307529.61
1803	390+92.18	20.21' RT	695.83	824254.21	307531.78
1804	390+96.97	21.64' RT	695.81	824250.21	307534.78
1805	391+02.12	25.20' RT	695.85	824246.60	307539.90
1806	391+04.90	28.75' RT	695.89	824245.22	307544.19
1807	391+06.95	33.75' RT	695.97	824245.02	307549.59
1808	391+07.46	37.80' RT	696.02	824245.95	307553.57
1809	391+07.55	45.90' RT	696.14	824248.66	307561.20
1810	391+04.36	33.75' RT	695.88	824247.45	307548.70
1811	391+01.90	28.75' RT	695.79	824248.04	307543.16
1812	390+97.42	28.75' RT	696.11	824252.24	307541.61
1813	390+92.42	28.75' RT	696.13	824256.93	307539.88
1814	390+87.36	28.75' RT	696.21	824261.67	307538.13
1815	390+82.36	31.03' RT	696.35	824267.15	307538.55
1816	390+82.36	33.56' RT	696.32	824268.03	307540.93
1817	390+87.36	33.75' RT	696.28	824263.40	307542.83
1818	390+92.42	33.75' RT	696.21	824258.66	307544.57
1819	390+92.48	39.55' RT	696.29	824260.61	307550.04
1820	390+92.20	44.56' RT	696.43	824262.59	307554.64
1821	390+96.08	44.52' RT	696.35	824258.94	307555.94
1822	390+97.48	39.50' RT	696.22	824255.90	307551.72
1823	390+97.42	33.75' RT	696.18	824253.96	307546.30
1824	391+01.90	33.75' RT	695.87	824249.76	307547.85



**LEGEND:**

- DIRECTION OF SLOPE
- .XXXX. SAWCUT REQ'D (SAWCUTS TO BE AT NEAREST EXISTING JOINT)
- [Pattern] DETECTABLE WARNING FIELD
- [Pattern] GUTTER PAN SLOPE TRANSITION (6.25% TO 4.0%) (UNLESS OTHERWISE NOTED)
- [Pattern] COLORED AND STAMPED CONCRETE SIDEWALK
- (SW4) CONCRETE SIDEWALK 4-INCH
- (G) 6:1 GRADED FLARE
- (C) 10:1 CONCRETE FLARE
- (PED) CONCRETE CURB PEDESTRIAN
- (ADJ) ADJUSTING INLET
- (COV) INSTALL NEW INLET COVER (TYPE DENOTED IN MISC. QUANT.)
- (4.0%) GUTTER PAN SLOPE (SLOPE DENOTED)
- (NO. ELEV.) POINT NUMBER & ELEVATION

1900 3RDGROVE ST					
POINT NO.	STATION	OFFSET	ELEV.	X	Y
1901	387+91.61	20.00' RT	695.95	824535.45	307433.87
1902	387+84.14	20.00' RT	695.89	824542.52	307432.30
1903	387+75.32	21.86' RT	695.89	824551.27	307432.37
1904	387+70.49	24.80' RT	695.89	824556.42	307434.35
1905	387+65.94	29.90' RT	695.93	824561.66	307438.53
1906	387+63.17	37.04' RT	695.98	824565.56	307445.07
1907	387+62.32	41.89' RT	696.01	824567.22	307449.70
1908	387+66.43	34.89' RT	695.88	824562.13	307443.52
1909	387+69.14	29.81' RT	695.83	824558.64	307439.02
1910	387+76.18	29.57' RT	696.31	824551.99	307440.09
1911	387+81.49	29.34' RT	696.38	824546.97	307440.89
1912	387+91.88	29.60' RT	696.45	824537.32	307443.28
1913	387+92.13	33.24' RT	696.53	824537.90	307446.88
1914	387+81.75	34.33' RT	696.41	824547.76	307445.83
1915	387+80.81	40.81' RT	696.58	824549.98	307451.99
1916	387+79.84	45.73' RT	696.75	824551.87	307456.62
1917	387+75.47	45.15' RT	696.67	824555.75	307455.22
1918	387+75.54	40.10' RT	696.50	824554.68	307450.29
1919	387+76.40	34.56' RT	696.38	824552.79	307445.02
1920	387+69.31	34.81' RT	695.91	824559.44	307443.96

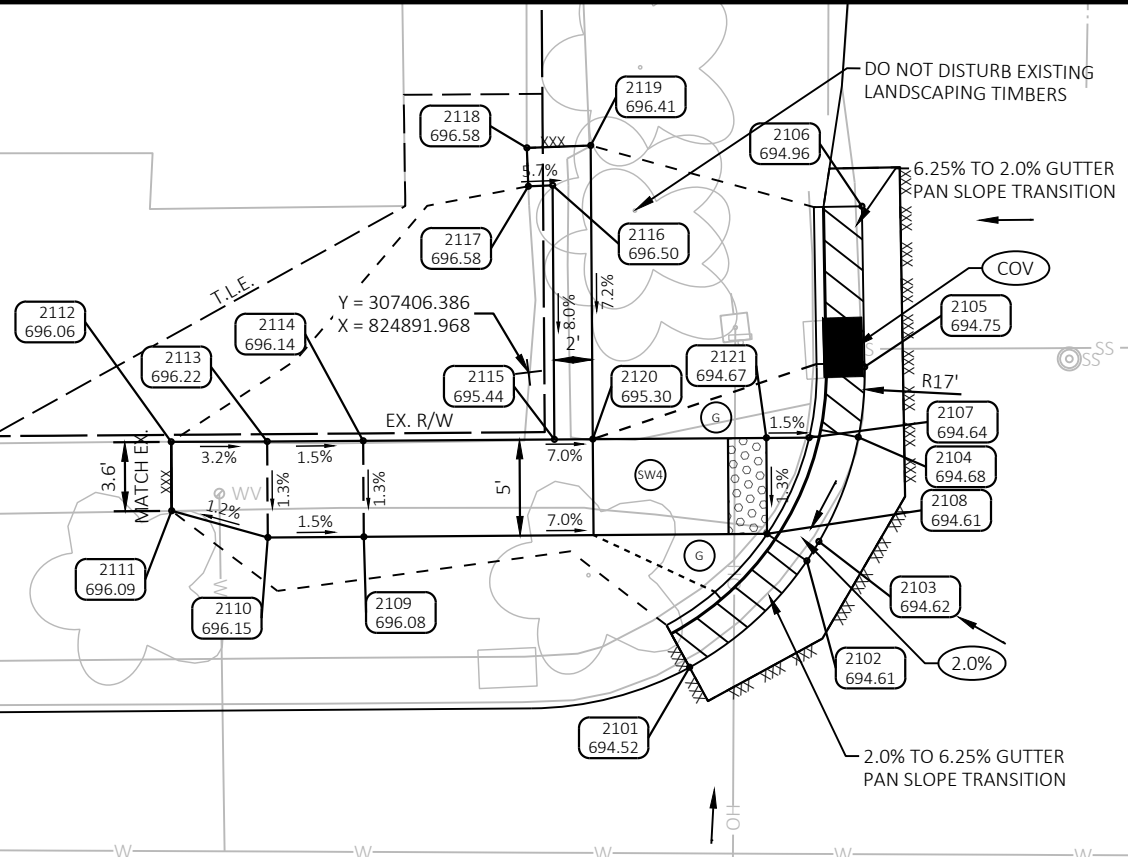


**LEGEND:**

- DIRECTION OF SLOPE
- .XXXX. SAWCUT REQ'D (SAWCUTS TO BE AT NEAREST EXISTING JOINT)
- [Pattern] DETECTABLE WARNING FIELD
- [Pattern] GUTTER PAN SLOPE TRANSITION (6.25% TO 4.0%) (UNLESS OTHERWISE NOTED)
- [Pattern] COLORED AND STAMPED CONCRETE SIDEWALK
- (SW4) CONCRETE SIDEWALK 4-INCH
- (G) 6:1 GRADED FLARE
- (C) 10:1 CONCRETE FLARE
- (PED) CONCRETE CURB PEDESTRIAN
- (ADJ) ADJUSTING INLET
- (COV) INSTALL NEW INLET COVER (TYPE DENOTED IN MISC. QUANT.)
- (4.0%) GUTTER PAN SLOPE (SLOPE DENOTED)
- (NO. ELEV.) POINT NUMBER & ELEVATION

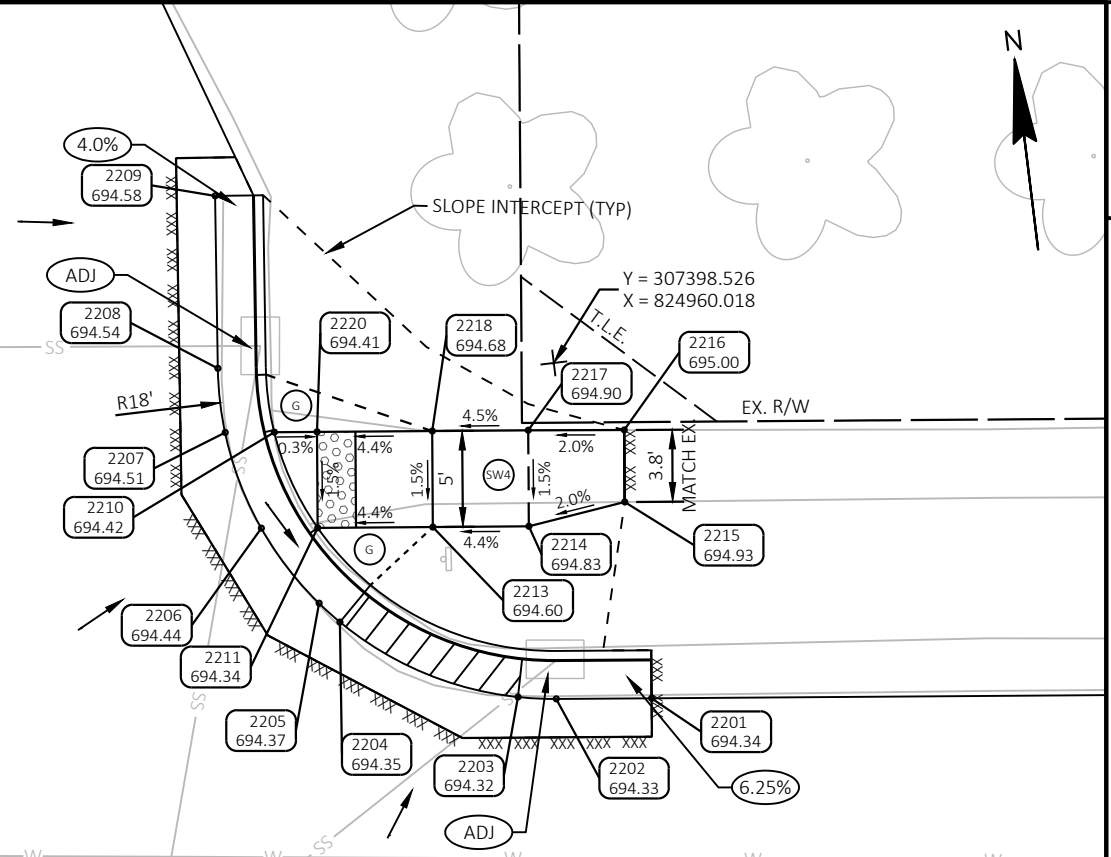
2000 3RDGROVE ST					
POINT NO.	STATION	OFFSET	ELEV.	X	Y
2001	387+06.19	20.00' RT	695.65	824617.56	307420.46
2002	387+11.69	20.77' RT	695.71	824612.18	307421.86
2003	387+19.97	25.43' RT	695.80	824604.57	307427.43
2004	387+23.62	29.72' RT	695.88	824601.62	307432.12
2005	387+25.93	34.80' RT	695.95	824600.06	307437.43
2006	387+26.84	41.04' RT	696.10	824600.01	307443.73
2007	387+23.15	34.71' RT	695.86	824602.67	307437.01
2008	387+20.38	29.61' RT	695.77	824604.67	307431.63
2009	387+14.27	29.35' RT	696.19	824610.60	307430.68
2010	387+09.28	29.13' RT	696.24	824615.53	307429.88
2011	387+04.21	30.53' RT	696.42	824620.73	307430.70
2012	387+04.04	34.34' RT	696.33	824621.33	307434.46
2013	387+09.06	34.12' RT	696.32	824616.32	307434.82
2014	387+14.06	34.27' RT	696.26	824611.38	307435.55
2015	387+20.17	34.60' RT	695.85	824605.46	307436.57

2100 3RD KING ST					
POINT NO.	STATION	OFFSET	ELEV.	X	Y
2101	384+23.70	22.08' RT	694.52	824898.42	307390.12
2102	384+17.55	27.58' RT	694.61	824905.15	307394.88
2103	384+16.91	28.58' RT	694.62	824905.90	307395.80
2104	384+14.83	34.00' RT	694.68	824908.60	307400.94
2105	384+14.47	37.66' RT	694.75	824909.37	307404.53
2106	384+14.55	46.02' RT	694.96	824910.25	307412.85
2107	384+17.39	34.00' RT	694.64	824906.06	307401.24
2108	384+19.61	29.00' RT	694.61	824903.27	307396.52
2109	384+40.61	29.00' RT	696.08	824882.41	307398.93
2110	384+45.61	29.00' RT	696.15	824877.44	307399.51
2111	384+50.61	30.42' RT	696.09	824872.64	307401.49
2112	384+50.61	34.02' RT	696.06	824873.05	307405.06
2113	384+45.61	34.00' RT	696.22	824878.02	307404.47
2114	384+40.61	34.00' RT	696.14	824882.98	307403.90
2115	384+30.65	34.00' RT	695.44	824892.88	307402.76
2116	384+30.65	47.23' RT	696.50	824894.40	307415.90
2117	384+31.92	47.19' RT	696.58	824893.14	307416.01
2118	384+31.98	49.19' RT	696.58	824893.30	307418.00
2119	384+28.65	49.29' RT	696.41	824896.62	307417.72
2120	384+28.65	34.00' RT	695.30	824894.87	307402.53
2121	384+19.61	34.00' RT	694.67	824903.84	307401.49



**CURB RAMP TYPE 4B  
NW QUADRANT**

KING ST



**CURB RAMP TYPE 4B  
NE QUADRANT**



3RD ST/STH 35

**LEGEND:**

- DIRECTION OF SLOPE
- .XXXX. SAWCUT REQ'D (SAWCUTS TO BE AT NEAREST EXISTING JOINT)
- DETECTABLE WARNING FIELD
- GUTTER PAN SLOPE TRANSITION (6.25% TO 4.0%) (UNLESS OTHERWISE NOTED)
- COLORED AND STAMPED CONCRETE SIDEWALK
- CONCRETE SIDEWALK 4-INCH
- 6:1 GRADED FLARE
- 10:1 CONCRETE FLARE
- CONCRETE CURB PEDESTRIAN
- ADJUSTING INLET
- INSTALL NEW INLET COVER (TYPE DENOTED IN MISC. QUANT.)
- GUTTER PAN SLOPE (SLOPE DENOTED)
- POINT NUMBER & ELEVATION

2200 3RD KING ST					
POINT NO.	STATION	OFFSET	ELEV.	X	Y
2201	383+58.51	20.00' RT	694.34	824962.94	307380.57
2202	383+63.47	20.00' RT	694.33	824958.01	307381.14
2203	383+65.46	20.11' RT	694.32	824956.04	307381.48
2204	383+74.71	24.08' RT	694.35	824947.32	307386.49
2205	383+75.79	25.07' RT	694.37	824946.35	307387.59
2206	383+78.77	29.00' RT	694.44	824943.85	307391.84
2207	383+80.62	34.00' RT	694.51	824942.58	307397.02
2208	383+80.97	37.34' RT	694.54	824942.62	307400.38
2209	383+81.05	46.32' RT	694.58	824943.57	307409.31
2210	383+78.06	34.00' RT	694.42	824945.13	307396.72
2211	383+75.83	29.00' RT	694.34	824946.76	307391.50
2213	383+69.83	29.00' RT	694.60	824952.73	307390.81
2214	383+64.83	29.00' RT	694.83	824957.69	307390.24
2215	383+59.83	30.23' RT	694.93	824962.80	307390.89
2216	383+59.83	33.98' RT	695.00	824963.23	307394.61
2217	383+64.83	34.00' RT	694.90	824958.27	307395.20
2218	383+69.83	34.00' RT	694.68	824953.30	307395.78
2220	383+75.83	34.00' RT	694.41	824947.34	307396.47

24118 3RD STREET  
NATIONAL REGISTER FOR HISTORIC PLACES

BLDG

Y = 307364.282  
X = 825256.488

SLOPE INTERCEPT (TYP)

GUY

T.L.E.

EX. R/W

2300 3RDSUMNER ST					
POINT NO.	STATION	OFFSET	ELEV.	X	Y
2301	380+57.06	21.92' RT	695.58	825262.62	307347.89
2302	380+50.82	27.29' RT	695.74	825269.44	307352.51
2303	380+47.94	33.75' RT	695.84	825273.04	307358.60
2304	380+47.53	37.40' RT	695.90	825273.86	307362.17
2305	380+47.47	48.15' RT	696.11	825275.16	307372.84
2306	380+50.51	33.75' RT	695.75	825270.49	307358.89
2307	380+52.85	28.75' RT	695.66	825267.59	307354.19
2308	380+58.85	28.75' RT	696.08	825261.63	307354.88
2309	380+63.85	28.75' RT	696.15	825256.66	307355.45
2310	380+68.85	28.75' RT	696.35	825251.69	307356.03
2311	380+73.85	30.28' RT	696.56	825246.90	307358.13
2312	380+73.85	33.95' RT	696.61	825247.32	307361.77
2313	380+68.85	33.75' RT	696.42	825252.27	307360.99
2314	380+63.85	33.75' RT	696.23	825257.23	307360.42
2315	380+58.85	33.75' RT	696.15	825262.20	307359.85
2316	380+52.85	33.75' RT	695.73	825268.16	307359.16

MINIMIZE IMPACTS TO  
LANDSCAPING. NOTIFY PROPERTY  
OWNER 7 CALENDAR DAY PRIOR  
TO COMMENCING WORK.

**CURB RAMP TYPE 4B  
NW QUADRANT**

SUMMER ST

**CURB RAMP TYPE 4B  
NE QUADRANT**

3RD ST/STH 35

2400 3RDSUMNER ST					
POINT NO.	STATION	OFFSET	ELEV.	X	Y
2401	379+99.02	21.06' RT	695.71	825320.17	307340.38
2402	380+06.57	26.42' RT	695.79	825313.29	307346.56
2403	380+07.72	28.00' RT	695.82	825312.32	307348.27
2404	380+09.94	33.00' RT	695.89	825310.70	307353.49
2405	380+10.52	37.72' RT	695.94	825310.67	307358.25
2406	380+10.47	47.93' RT	696.05	825311.89	307368.38
2407	380+07.34	33.00' RT	695.80	825313.28	307353.19
2408	380+04.64	28.00' RT	695.71	825315.39	307347.92
2409	379+98.64	28.00' RT	696.13	825321.35	307347.23
2410	379+83.64	28.00' RT	697.18	825336.25	307345.51
2411	379+78.64	28.00' RT	697.26	825341.22	307344.93
2412	379+73.64	28.59' RT	697.36	825346.25	307344.94
2413	379+73.64	32.43' RT	697.30	825346.70	307348.76
2414	379+78.64	33.00' RT	697.33	825341.79	307349.90
2415	379+83.64	33.00' RT	697.26	825336.83	307350.47
2416	379+98.64	33.00' RT	696.21	825321.93	307352.19
2417	380+04.64	33.00' RT	695.79	825315.97	307352.88

**LEGEND:**

- DIRECTION OF SLOPE
- .XXXX. SAWCUT REQ'D (SAWCUTS TO BE AT NEAREST EXISTING JOINT)
- DETECTABLE WARNING FIELD
- GUTTER PAN SLOPE TRANSITION (6.25% TO 4.0%) (UNLESS OTHERWISE NOTED)
- COLORED AND STAMPED CONCRETE SIDEWALK
- CONCRETE SIDEWALK 4-INCH
- 6:1 GRADED FLARE
- 10:1 CONCRETE FLARE
- CONCRETE CURB PEDESTRIAN
- ADJUSTING INLET
- INSTALL NEW INLET COVER (TYPE DENOTED IN MISC. QUANT.)
- GUTTER PAN SLOPE (SLOPE DENOTED)
- POINT NUMBER & ELEVATION

PROJECT NO: 7140-00-70

HWY: STH 35

COUNTY: TREMPLEAU

CURB RAMP DETAILS: SUMNER ST

SHEET

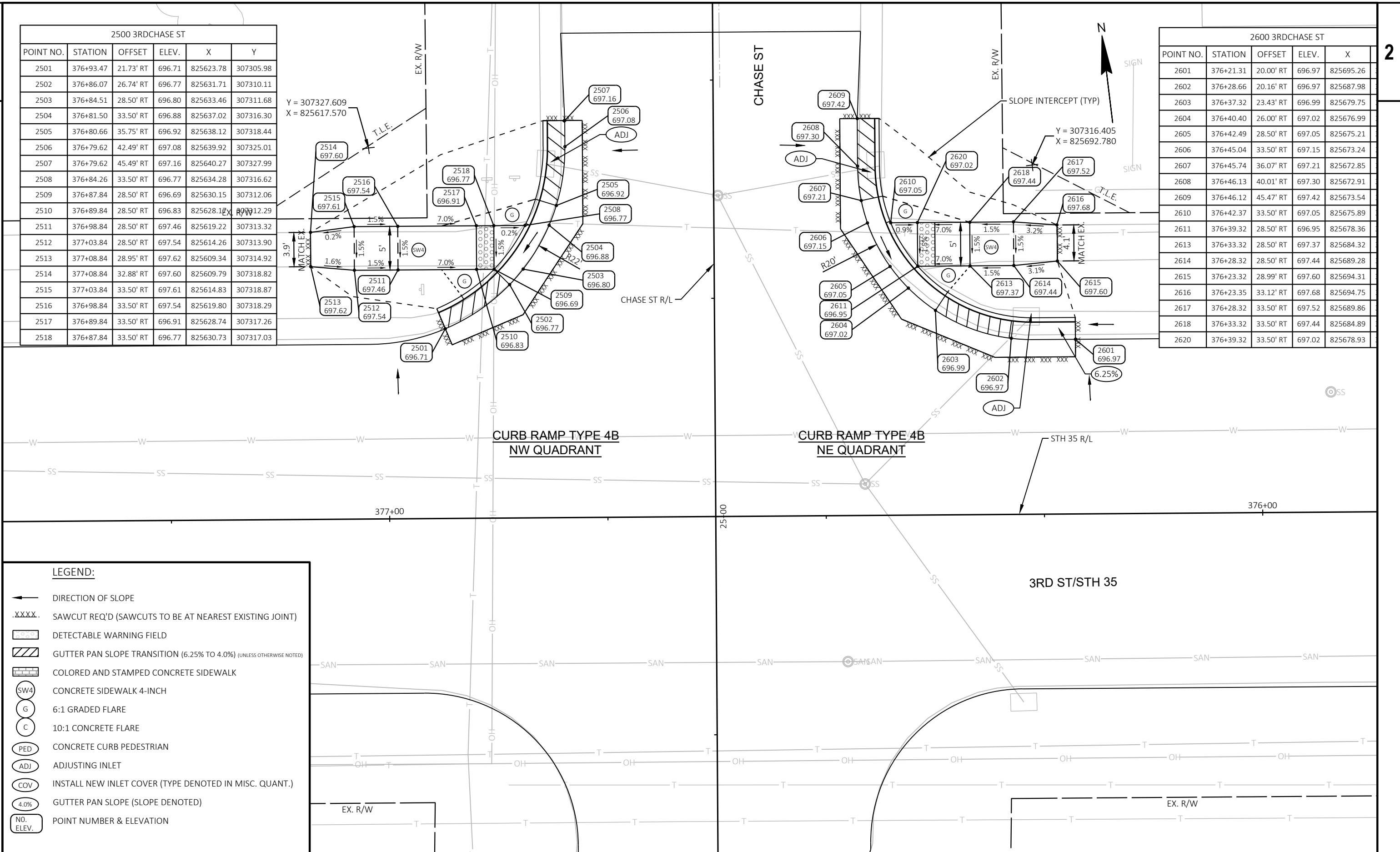
E

2500 3RDCHASE ST					
POINT NO.	STATION	OFFSET	ELEV.	X	Y
2501	376+93.47	21.73' RT	696.71	825623.78	307305.98
2502	376+86.07	26.74' RT	696.77	825631.71	307310.11
2503	376+84.51	28.50' RT	696.80	825633.46	307311.68
2504	376+81.50	33.50' RT	696.88	825637.02	307316.30
2505	376+80.66	35.75' RT	696.92	825638.12	307318.44
2506	376+79.62	42.49' RT	697.08	825639.92	307325.01
2507	376+79.62	45.49' RT	697.16	825640.27	307327.99
2508	376+84.26	33.50' RT	696.77	825634.28	307316.62
2509	376+87.84	28.50' RT	696.69	825630.15	307312.06
2510	376+89.84	28.50' RT	696.83	825628.12	307312.29
2511	376+98.84	28.50' RT	697.46	825619.22	307313.32
2512	377+03.84	28.50' RT	697.54	825614.26	307313.90
2513	377+08.84	28.95' RT	697.62	825609.34	307314.92
2514	377+08.84	32.88' RT	697.60	825609.79	307318.82
2515	377+03.84	33.50' RT	697.61	825614.83	307318.87
2516	376+98.84	33.50' RT	697.54	825619.80	307318.29
2517	376+89.84	33.50' RT	696.91	825628.74	307317.26
2518	376+87.84	33.50' RT	696.77	825630.73	307317.03

2600 3RDCHASE ST					
POINT NO.	STATION	OFFSET	ELEV.	X	Y
2601	376+21.31	20.00' RT	696.97	825695.26	
2602	376+28.66	20.16' RT	696.97	825687.98	
2603	376+37.32	23.43' RT	696.99	825679.75	
2604	376+40.40	26.00' RT	697.02	825676.99	
2605	376+42.49	28.50' RT	697.05	825675.21	
2606	376+45.04	33.50' RT	697.15	825673.24	
2607	376+45.74	36.07' RT	697.21	825672.85	
2608	376+46.13	40.01' RT	697.30	825672.91	
2609	376+46.12	45.47' RT	697.42	825673.54	
2610	376+42.37	33.50' RT	697.05	825675.89	
2611	376+39.32	28.50' RT	696.95	825678.36	
2613	376+33.32	28.50' RT	697.37	825684.32	
2614	376+28.32	28.50' RT	697.44	825689.28	
2615	376+23.32	28.99' RT	697.60	825694.31	
2616	376+23.35	33.12' RT	697.68	825694.75	
2617	376+28.32	33.50' RT	697.52	825689.86	
2618	376+33.32	33.50' RT	697.44	825684.89	
2620	376+39.32	33.50' RT	697.02	825678.93	

Y = 307327.609  
X = 825617.570

Y = 307316.405  
X = 825692.780

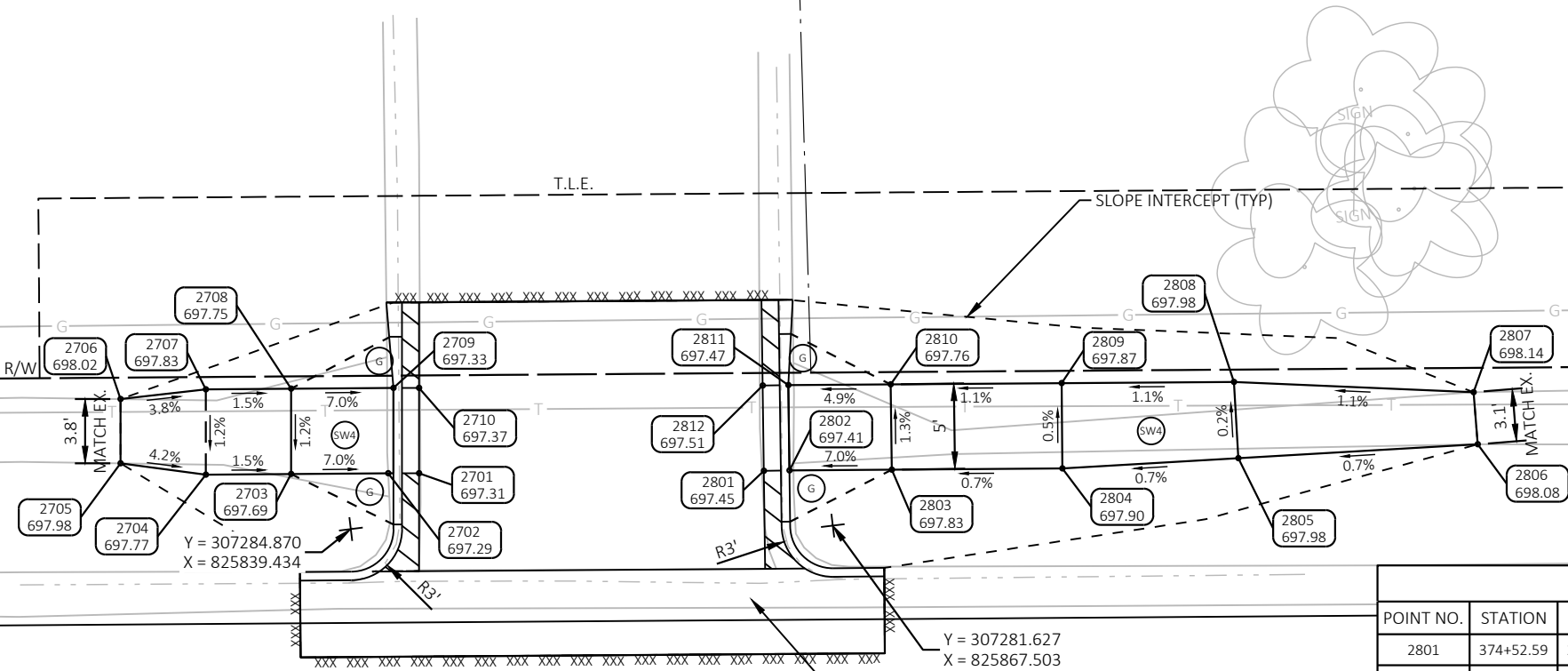


**LEGEND:**

- DIRECTION OF SLOPE
- .XXXX. SAWCUT REQ'D (SAWCUTS TO BE AT NEAREST EXISTING JOINT)
- [Pattern] DETECTABLE WARNING FIELD
- [Pattern] GUTTER PAN SLOPE TRANSITION (6.25% TO 4.0%) (UNLESS OTHERWISE NOTED)
- [Pattern] COLORED AND STAMPED CONCRETE SIDEWALK
- (SW4) CONCRETE SIDEWALK 4-INCH
- (G) 6:1 GRADED FLARE
- (C) 10:1 CONCRETE FLARE
- (PED) CONCRETE CURB PEDESTRIAN
- (ADJ) ADJUSTING INLET
- (COV) INSTALL NEW INLET COVER (TYPE DENOTED IN MISC. QUANT.)
- (4.0%) GUTTER PAN SLOPE (SLOPE DENOTED)
- (NO. ELEV.) POINT NUMBER & ELEVATION

2700 3RDMID					
POINT NO.	STATION	OFFSET	ELEV.	X	Y
2701	374+72.80	28.74' RT	697.31	825843.80	307287.63
2702	374+74.61	28.75' RT	697.29	825842.01	307287.84
2703	374+80.30	28.75' RT	697.69	825836.35	307288.50
2704	374+85.30	28.75' RT	697.77	825831.39	307289.07
2705	374+90.29	29.45' RT	697.98	825826.50	307290.34
2706	374+90.27	33.21' RT	698.02	825826.96	307294.08
2707	374+85.26	33.75' RT	697.83	825831.99	307294.03
2708	374+80.26	33.75' RT	697.75	825836.96	307293.46
2709	374+74.26	33.75' RT	697.33	825842.92	307292.77
2710	374+72.76	33.74' RT	697.37	825844.41	307292.59

2800 3RDMID					
POINT NO.	STATION	OFFSET	ELEV.	X	Y
2801	374+52.59	28.74' RT	697.45	825863.88	307285.31
2802	374+51.09	28.75' RT	697.41	825865.37	307285.15
2803	374+45.09	28.75' RT	697.83	825871.33	307284.46
2804	374+35.09	28.75' RT	697.90	825881.27	307283.31
2805	374+24.77	29.28' RT	697.98	825891.58	307282.65
2806	374+10.73	29.99' RT	698.08	825905.61	307281.75
2807	374+10.95	33.04' RT	698.14	825905.74	307284.81
2808	374+24.99	33.75' RT	697.98	825891.87	307287.12
2809	374+35.09	33.75' RT	697.87	825881.84	307288.28
2810	374+45.12	33.75' RT	697.76	825871.87	307289.43
2811	374+51.12	33.75' RT	697.47	825865.91	307290.12
2812	374+52.62	33.74' RT	697.51	825864.42	307290.28



**CURB RAMP TYPE 4B  
NW QUADRANT**

**CURB RAMP TYPE 4B  
NW QUADRANT**

**3RD ST/STH 35**

STH 35 R/L

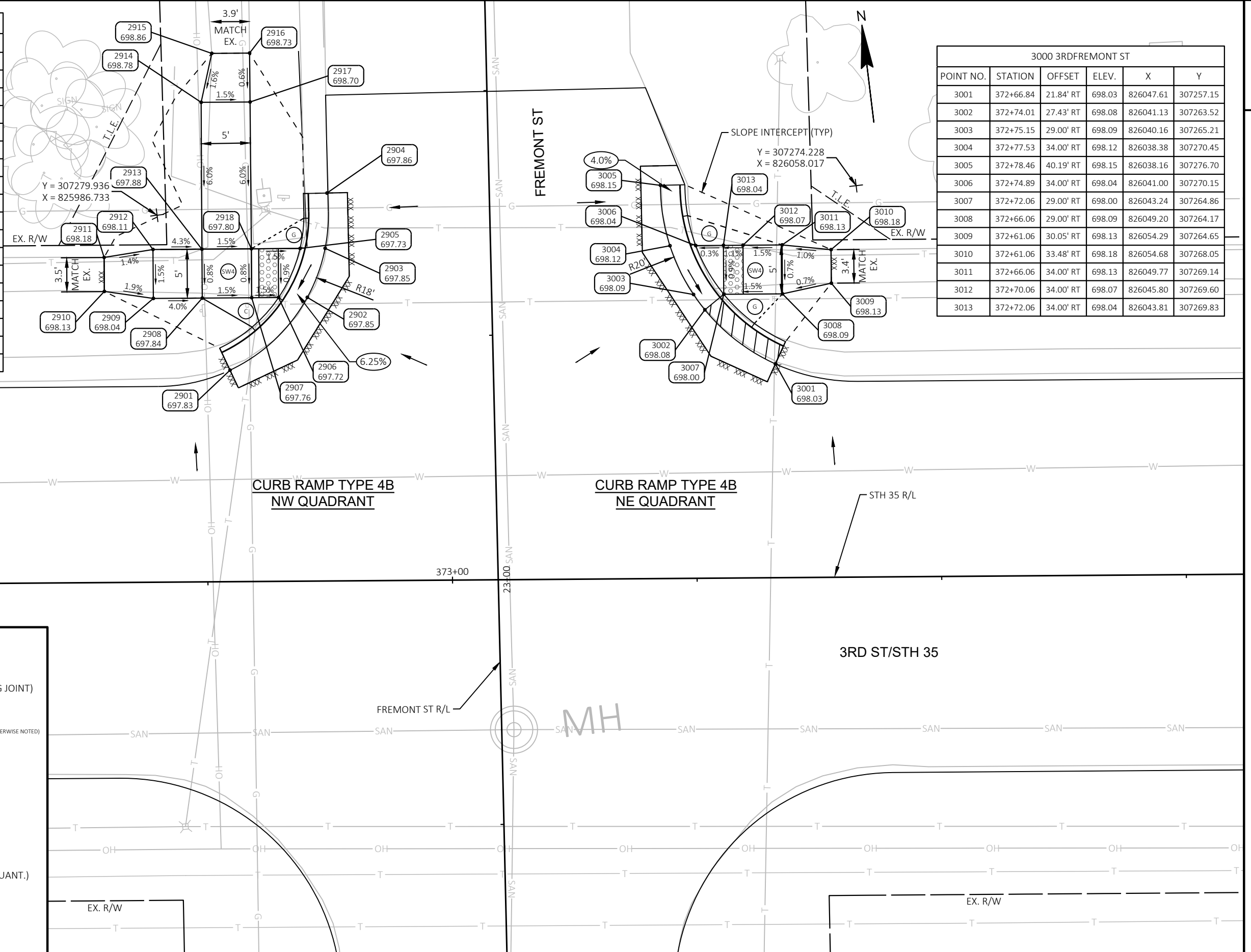
**LEGEND:**

- DIRECTION OF SLOPE
- .XXXX. SAWCUT REQ'D (SAWCUTS TO BE AT NEAREST EXISTING JOINT)
- [Pattern] DETECTABLE WARNING FIELD
- [Pattern] GUTTER PAN SLOPE TRANSITION (6.25% TO 4.0%) (UNLESS OTHERWISE NOTED)
- [Pattern] COLORED AND STAMPED CONCRETE SIDEWALK
- (SW4) CONCRETE SIDEWALK 4-INCH
- (G) 6:1 GRADED FLARE
- (C) 10:1 CONCRETE FLARE
- (PED) CONCRETE CURB PEDESTRIAN
- (ADJ) ADJUSTING INLET
- (COV) INSTALL NEW INLET COVER (TYPE DENOTED IN MISC. QUANT.)
- (4.0%) GUTTER PAN SLOPE (SLOPE DENOTED)
- (NO. ELEV.) POINT NUMBER & ELEVATION



2900 3RDFREMONT ST					
POINT NO.	STATION	OFFSET	ELEV.	X	Y
2901	373+22.58	21.62' RT	697.83	825992.21	307263.31
2902	373+14.63	29.00' RT	697.85	826000.95	307269.74
2903	373+12.78	34.00' RT	697.85	826003.36	307274.49
2904	373+12.55	39.63' RT	697.86	826004.24	307280.06
2905	373+15.34	34.00' RT	697.73	826000.82	307274.79
2906	373+17.58	29.01' RT	697.72	825998.03	307270.08
2907	373+20.32	29.00' RT	697.76	825995.30	307270.39
2908	373+25.37	29.00' RT	697.84	825990.28	307270.97
2909	373+30.37	29.00' RT	698.04	825985.32	307271.54
2910	373+35.37	29.76' RT	698.13	825980.44	307272.87
2911	373+35.37	33.23' RT	698.18	825980.84	307276.32
2912	373+30.37	34.00' RT	698.11	825985.89	307276.51
2913	373+25.37	34.00' RT	697.88	825990.86	307275.94
2914	373+25.35	49.00' RT	698.78	825992.61	307290.84
2915	373+24.22	54.00' RT	698.86	825994.29	307295.67
2916	373+20.35	54.00' RT	698.73	825998.14	307295.23
2917	373+20.35	49.00' RT	698.70	825997.57	307290.26
2918	373+20.32	34.00' RT	697.80	825995.88	307275.36

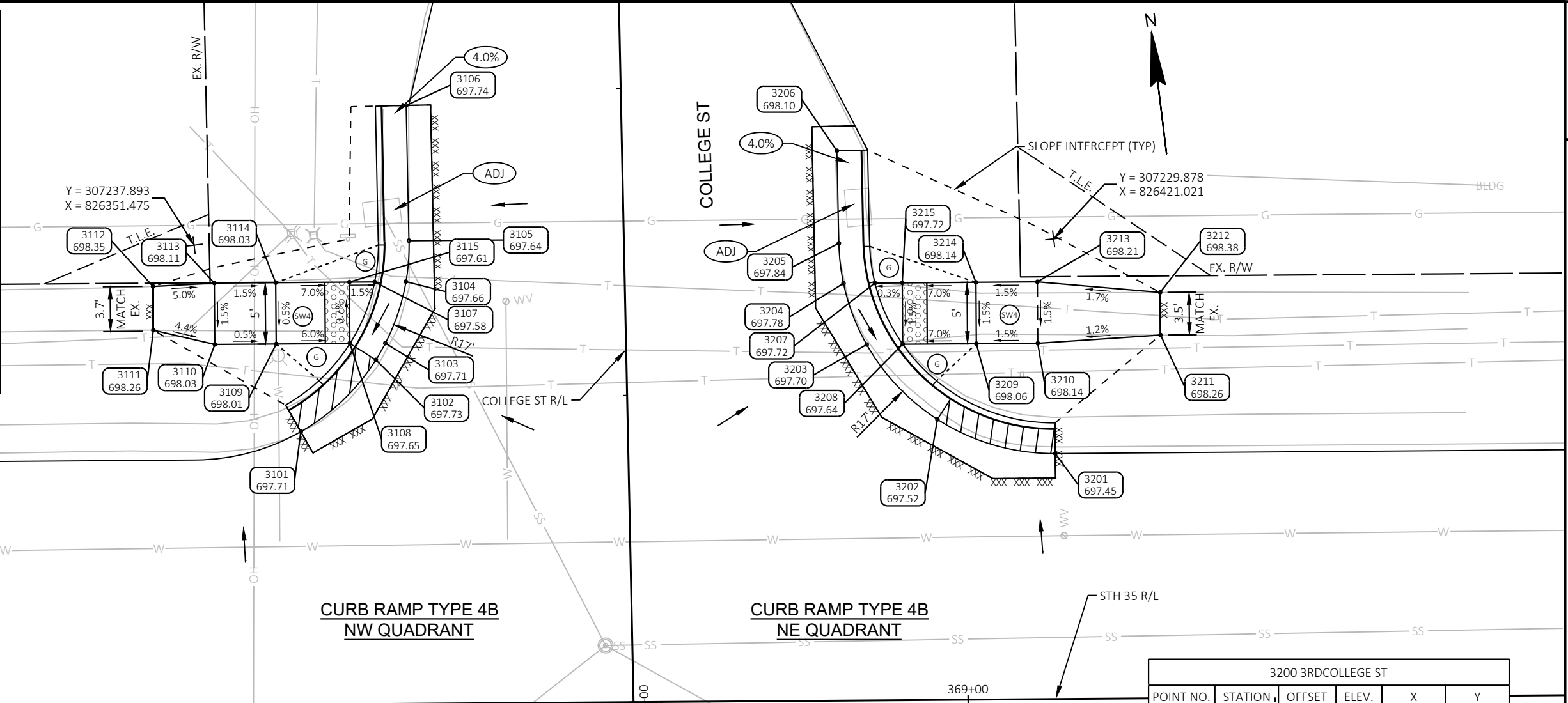
3000 3RDFREMONT ST					
POINT NO.	STATION	OFFSET	ELEV.	X	Y
3001	372+66.84	21.84' RT	698.03	826047.61	307257.15
3002	372+74.01	27.43' RT	698.08	826041.13	307263.52
3003	372+75.15	29.00' RT	698.09	826040.16	307265.21
3004	372+77.53	34.00' RT	698.12	826038.38	307270.45
3005	372+78.46	40.19' RT	698.15	826038.16	307276.70
3006	372+74.89	34.00' RT	698.04	826041.00	307270.15
3007	372+72.06	29.00' RT	698.00	826043.24	307264.86
3008	372+66.06	29.00' RT	698.09	826049.20	307264.17
3009	372+61.06	30.05' RT	698.13	826054.29	307264.65
3010	372+61.06	33.48' RT	698.18	826054.68	307268.05
3011	372+66.06	34.00' RT	698.13	826049.77	307269.14
3012	372+70.06	34.00' RT	698.07	826045.80	307269.60
3013	372+72.06	34.00' RT	698.04	826043.81	307269.83



**LEGEND:**

- DIRECTION OF SLOPE
- .XXXX. SAWCUT REQ'D (SAWCUTS TO BE AT NEAREST EXISTING JOINT)
- DETECTABLE WARNING FIELD
- GUTTER PAN SLOPE TRANSITION (6.25% TO 4.0%) (UNLESS OTHERWISE NOTED)
- COLORED AND STAMPED CONCRETE SIDEWALK
- CONCRETE SIDEWALK 4-INCH
- 6:1 GRADED FLARE
- 10:1 CONCRETE FLARE
- CONCRETE CURB PEDESTRIAN
- ADJUSTING INLET
- INSTALL NEW INLET COVER (TYPE DENOTED IN MISC. QUANT.)
- GUTTER PAN SLOPE (SLOPE DENOTED)
- POINT NUMBER & ELEVATION

3100 3RDCOLLEGE ST					
POINT NO.	STATION	OFFSET	ELEV.	X	Y
3101	369+54.19	22.24' RT	697.71	826358.24	307221.76
3102	369+48.03	28.05' RT	697.73	826365.03	307226.82
3103	369+47.25	29.40' RT	697.71	826365.96	307228.07
3104	369+45.54	34.40' RT	697.66	826368.23	307232.84
3105	369+45.26	37.74' RT	697.64	826368.89	307236.13
3106	369+45.41	48.76' RT	697.74	826370.00	307247.09
3107	369+48.09	34.40' RT	697.58	826365.70	307233.13
3108	369+50.14	29.40' RT	697.65	826363.09	307228.40
3109	369+56.14	29.40' RT	698.01	826357.13	307229.09
3110	369+61.14	29.40' RT	698.03	826352.16	307229.66
3111	369+66.14	30.60' RT	698.26	826347.33	307231.42
3112	369+66.14	34.17' RT	698.35	826347.74	307234.97
3113	369+61.14	34.40' RT	698.11	826352.73	307234.63
3114	369+56.14	34.40' RT	698.03	826357.70	307234.05
3115	369+50.14	34.40' RT	697.61	826363.66	307233.37

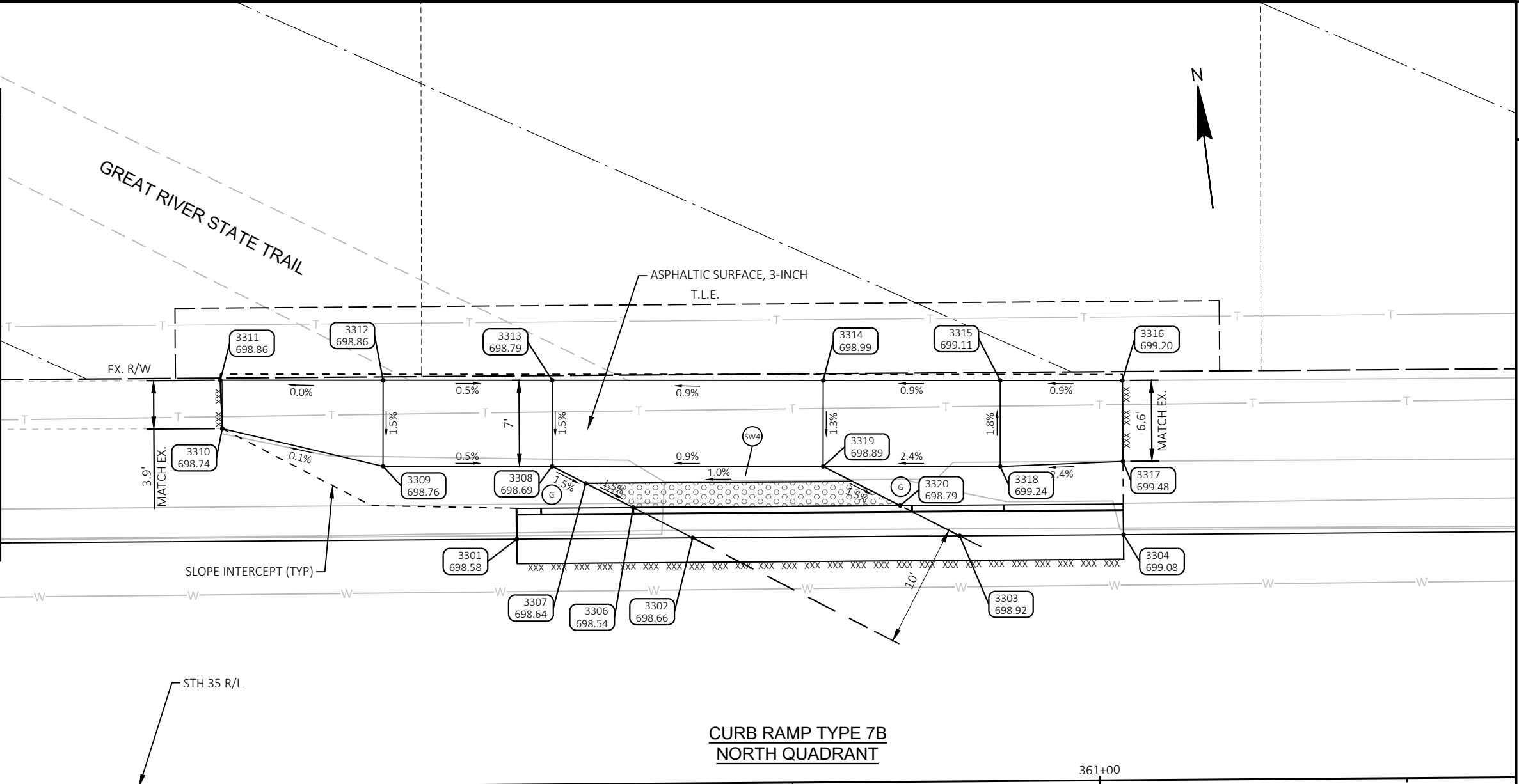


**LEGEND:**

- DIRECTION OF SLOPE
- .XXXX. SAWCUT REQ'D (SAWCUTS TO BE AT NEAREST EXISTING JOINT)
- [Pattern] DETECTABLE WARNING FIELD
- [Pattern] GUTTER PAN SLOPE TRANSITION (6.25% TO 4.0%) (UNLESS OTHERWISE NOTED)
- [Pattern] COLORED AND STAMPED CONCRETE SIDEWALK
- (SW4) CONCRETE SIDEWALK 4-INCH
- (G) 6:1 GRADED FLARE
- (C) 10:1 CONCRETE FLARE
- (PED) CONCRETE CURB PEDESTRIAN
- (ADJ) ADJUSTING INLET
- (COV) INSTALL NEW INLET COVER (TYPE DENOTED IN MISC. QUANT.)
- (4.0%) GUTTER PAN SLOPE (SLOPE DENOTED)
- (NO. ELEV.) POINT NUMBER & ELEVATION

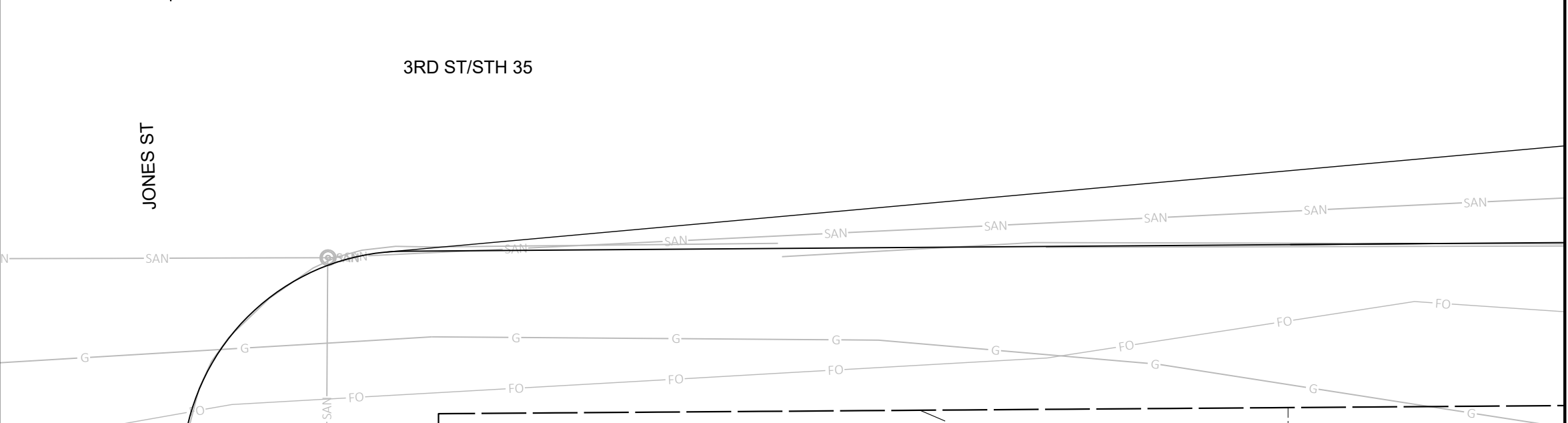
3200 3RDCOLLEGE ST					
POINT NO.	STATION	OFFSET	ELEV.	X	Y
3201	368+92.76	20.00' RT	697.45	826419.02	307212.49
3202	369+02.34	22.86' RT	697.52	826409.82	307216.43
3203	369+08.05	29.00' RT	697.70	826404.85	307223.19
3204	369+09.90	34.00' RT	697.78	826403.59	307228.36
3205	369+10.25	37.26' RT	697.84	826403.61	307231.65
3206	369+10.36	44.81' RT	698.10	826404.37	307239.15
3207	369+07.34	34.00' RT	697.72	826406.13	307228.07
3208	369+05.12	29.00' RT	697.64	826407.77	307222.85
3209	368+99.12	29.00' RT	698.06	826413.73	307222.16
3210	368+94.12	29.00' RT	698.14	826418.70	307221.59
3211	368+84.12	29.60' RT	698.26	826428.70	307221.04
3212	368+84.12	33.07' RT	698.38	826429.10	307224.49
3213	368+94.12	34.00' RT	698.21	826419.27	307226.56
3214	368+99.12	34.00' RT	698.14	826414.30	307227.13
3215	369+05.12	34.00' RT	697.72	826408.34	307227.82

3300 3RDTRAIL					
POINT NO.	STATION	OFFSET	ELEV.	X	Y
3301	361+47.32	20.00' RT	698.58	827159.56	307127.15
3302	361+32.99	20.00' RT	698.66	827173.78	307125.51
3303	361+11.27	20.00' RT	698.92	827195.37	307123.03
3304	360+97.93	20.00' RT	699.08	827208.62	307121.50
3306	361+37.82	22.50' RT	698.54	827169.28	307128.55
3307	361+41.68	24.50' RT	698.64	827165.68	307130.98
3308	361+44.39	25.91' RT	698.69	827163.14	307132.69
3309	361+58.16	26.02' RT	698.76	827149.48	307134.37
3310	361+71.22	29.18' RT	698.74	827136.86	307139.00
3311	361+71.35	33.12' RT	698.86	827137.18	307142.94
3312	361+58.10	33.02' RT	698.86	827150.33	307141.32
3313	361+44.34	32.91' RT	698.79	827163.99	307139.63
3314	361+22.28	32.74' RT	698.99	827185.89	307136.94
3315	361+07.87	32.63' RT	699.11	827200.19	307135.18
3316	360+97.93	32.55' RT	699.20	827210.06	307133.96
3317	360+97.93	25.97' RT	699.48	827209.30	307127.43
3318	361+07.93	25.63' RT	699.24	827199.33	307128.23
3319	361+22.33	25.74' RT	698.89	827185.03	307129.99
3320	361+16.09	22.50' RT	698.79	827190.86	307126.06



LEGEND:

- DIRECTION OF SLOPE
- .XXXX. SAWCUT REQ'D (SAWCUTS TO BE AT NEAREST EXISTING JOINT)
- DETECTABLE WARNING FIELD
- GUTTER PAN SLOPE TRANSITION (6.25% TO 4.0%) (UNLESS OTHERWISE NOTED)
- COLORED AND STAMPED CONCRETE SIDEWALK
- CONCRETE SIDEWALK 4-INCH
- 6:1 GRADED FLARE
- 10:1 CONCRETE FLARE
- CONCRETE CURB PEDESTRIAN
- ADJUSTING INLET
- INSTALL NEW INLET COVER (TYPE DENOTED IN MISC. QUANT.)
- GUTTER PAN SLOPE (SLOPE DENOTED)
- POINT NUMBER & ELEVATION



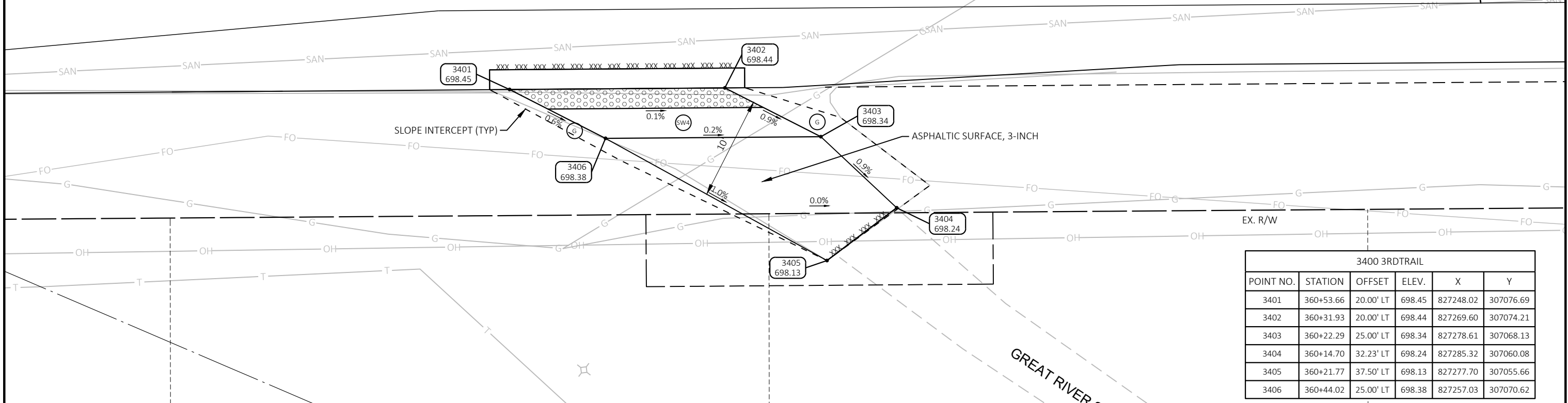
361+00

360+00

3RD ST/STH 35

**CURB RAMP TYPE 7B  
SOUTH QUADRANT**

STH 35 R/L



3400 3RDTRAIL					
POINT NO.	STATION	OFFSET	ELEV.	X	Y
3401	360+53.66	20.00' LT	698.45	827248.02	307076.69
3402	360+31.93	20.00' LT	698.44	827269.60	307074.21
3403	360+22.29	25.00' LT	698.34	827278.61	307068.13
3404	360+14.70	32.23' LT	698.24	827285.32	307060.08
3405	360+21.77	37.50' LT	698.13	827277.70	307055.66
3406	360+44.02	25.00' LT	698.38	827257.03	307070.62

**LEGEND:**

- DIRECTION OF SLOPE
- .XXXX. SAWCUT REQ'D (SAWCUTS TO BE AT NEAREST EXISTING JOINT)
- DETECTABLE WARNING FIELD
- GUTTER PAN SLOPE TRANSITION (6.25% TO 4.0%) (UNLESS OTHERWISE NOTED)
- COLORED AND STAMPED CONCRETE SIDEWALK
- CONCRETE SIDEWALK 4-INCH
- 6:1 GRADED FLARE
- 10:1 CONCRETE FLARE
- CONCRETE CURB PEDESTRIAN
- ADJUSTING INLET
- INSTALL NEW INLET COVER (TYPE DENOTED IN MISC. QUANT.)
- GUTTER PAN SLOPE (SLOPE DENOTED)
- POINT NUMBER & ELEVATION

PROJECT NO: 7140-00-70

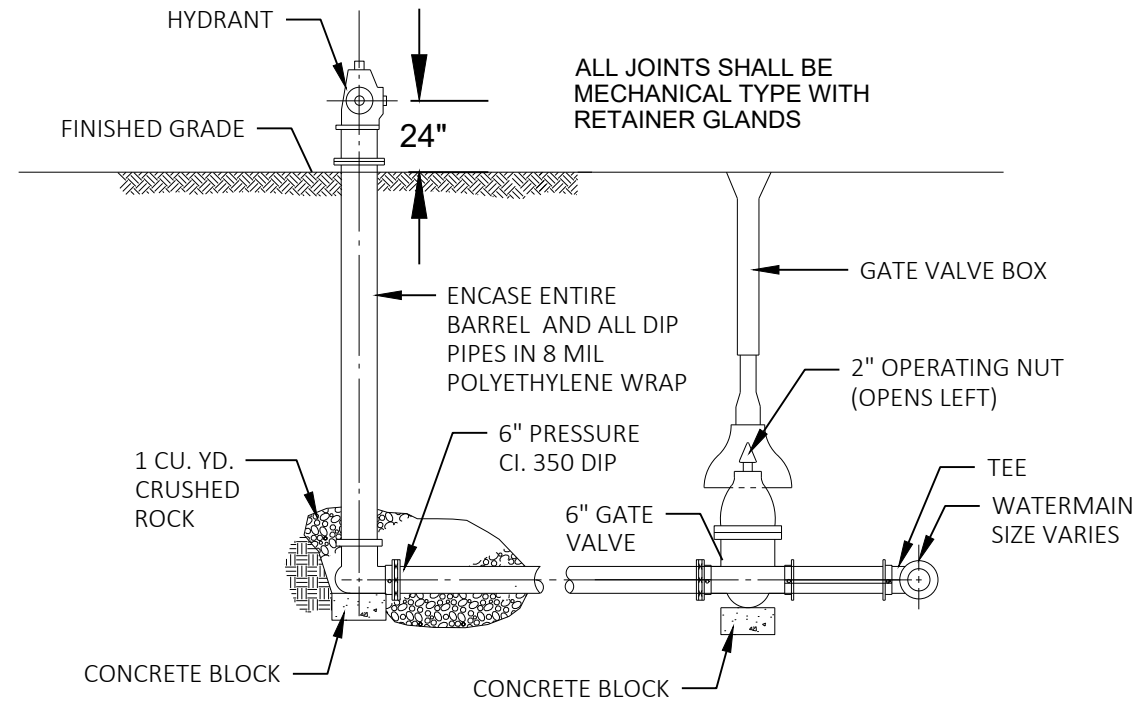
HWY: STH 35

COUNTY: TREMPLEAU

CURB RAMP DETAILS: GREAT RIVER STATE TRAIL

SHEET

E

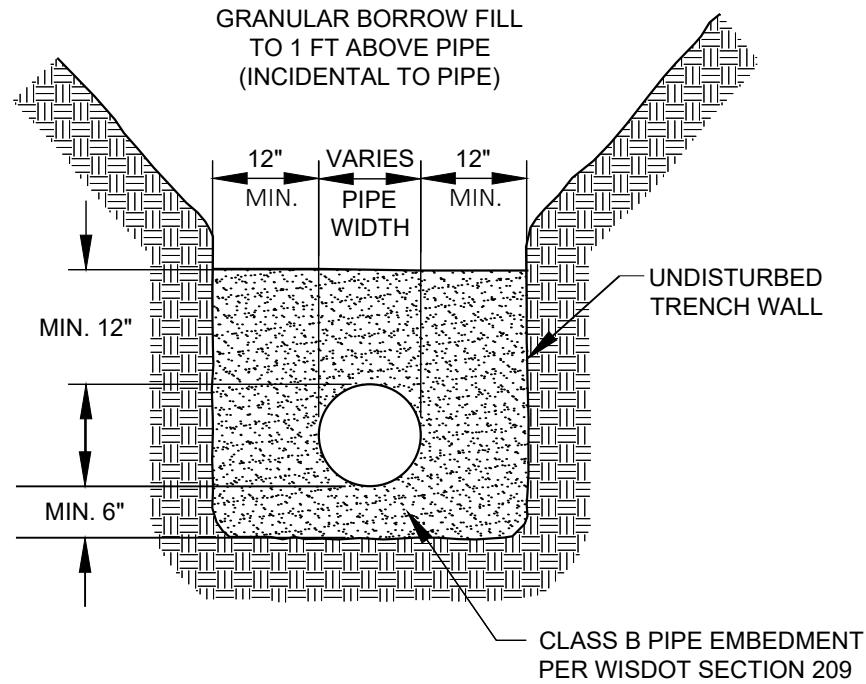


**FIRE HYDRANT LEAD**

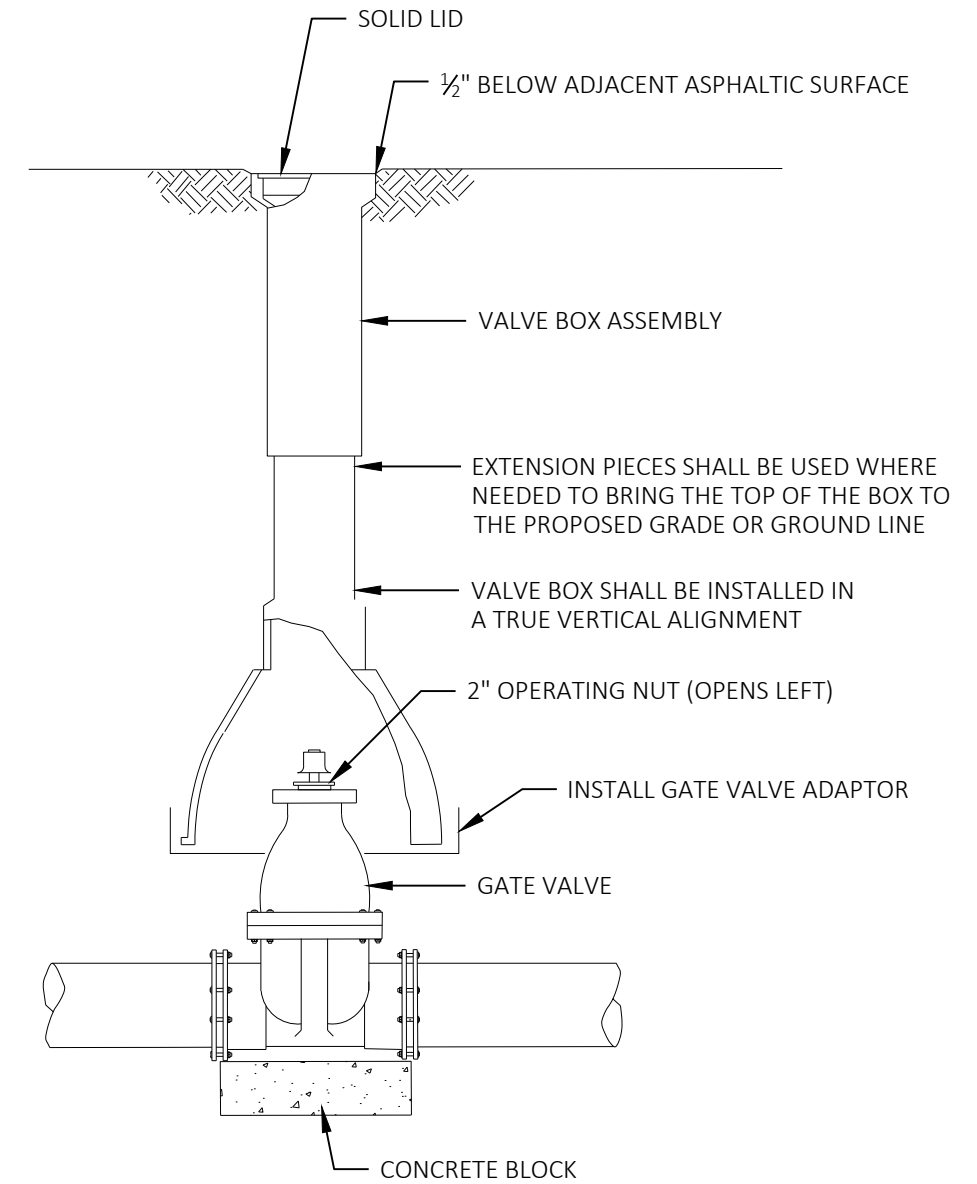
NOTES:

CLASS B PIPE EMBEDMENT SHALL BE INCIDENTAL TO ALL PIPE INSTALLATION. SALVAGED CRUSHED AGGREGATE BASE COURSE MAY BE USED.

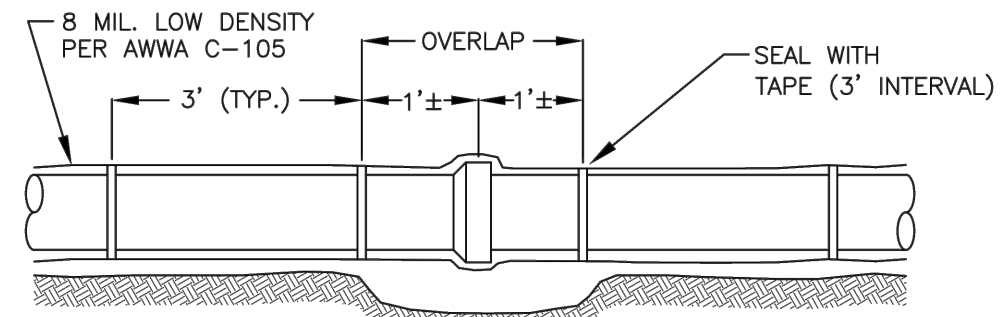
PERFORM TRENCHING, SHORING, AND EXCAVATING ACCORDING TO 29 CFR PART 1926, OSHA SUBPART P. USE SHORING IF UTILITIES AND OTHER RESTRAINTS MAKE SLOPING OR BENCHING OF THE EXCAVATION IMPRACTICABLE.



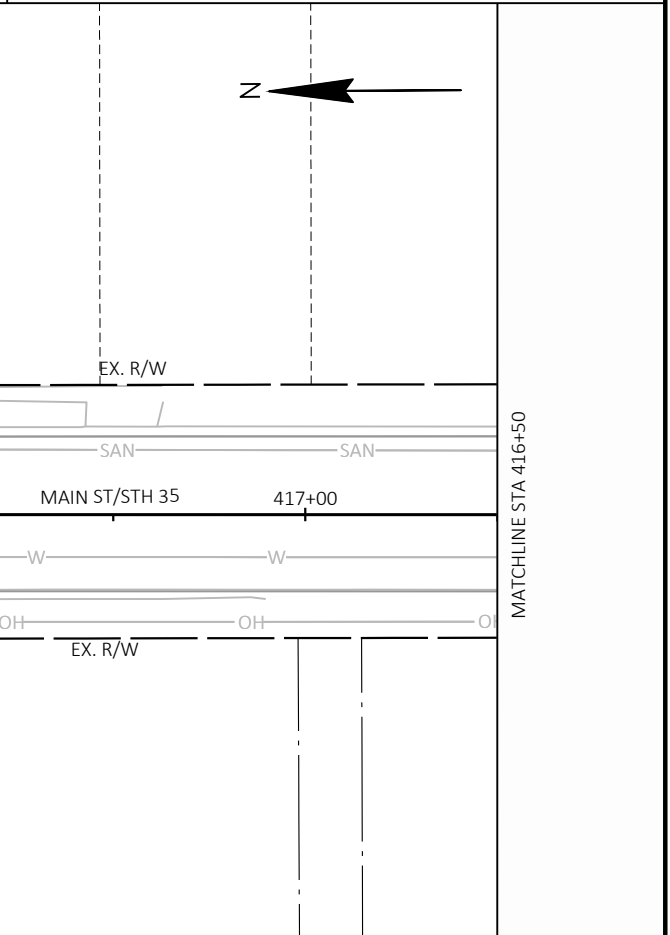
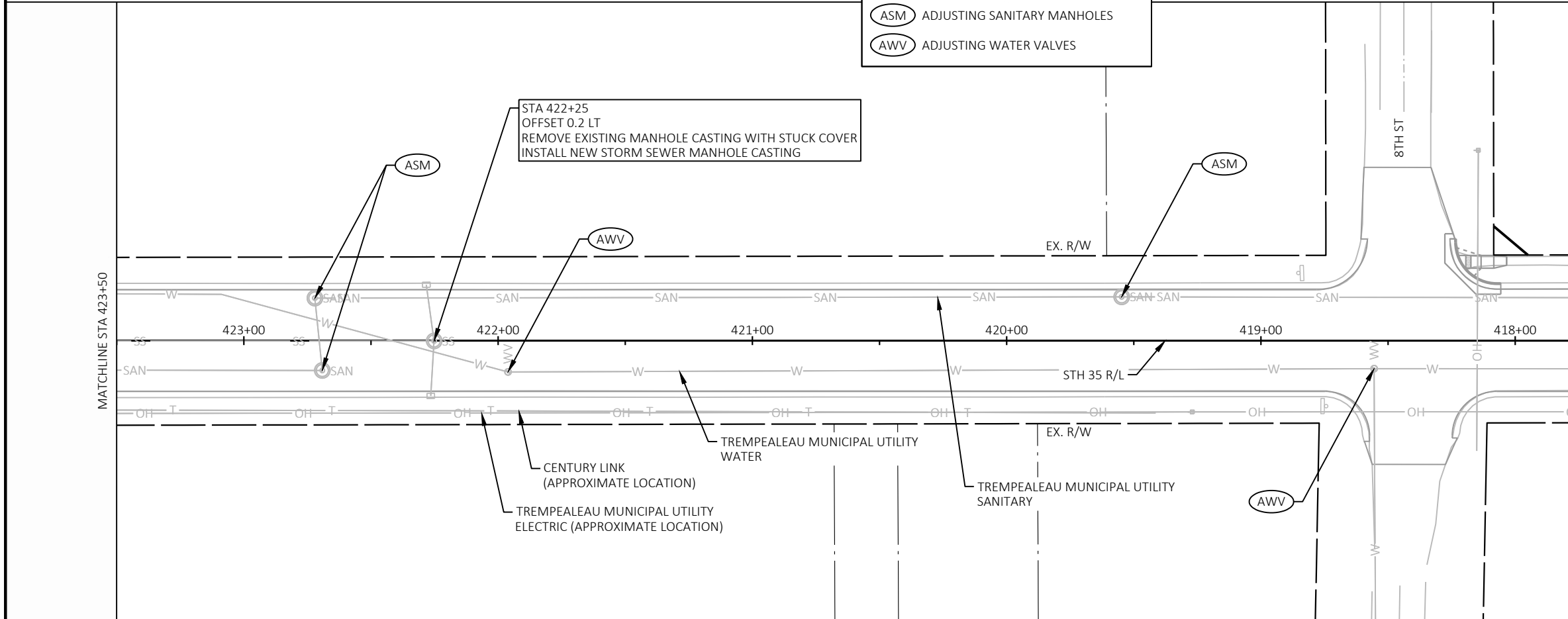
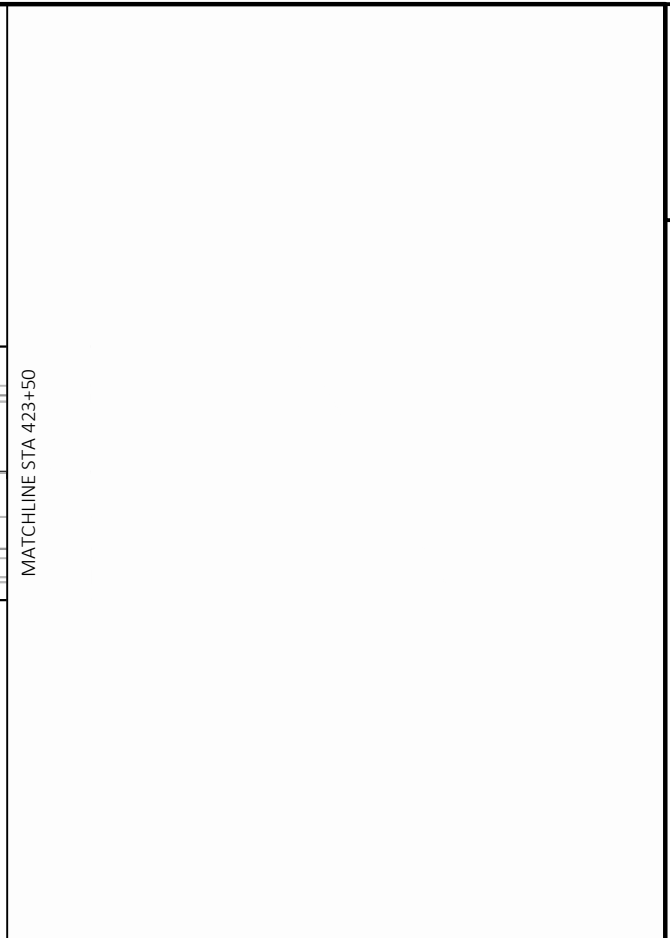
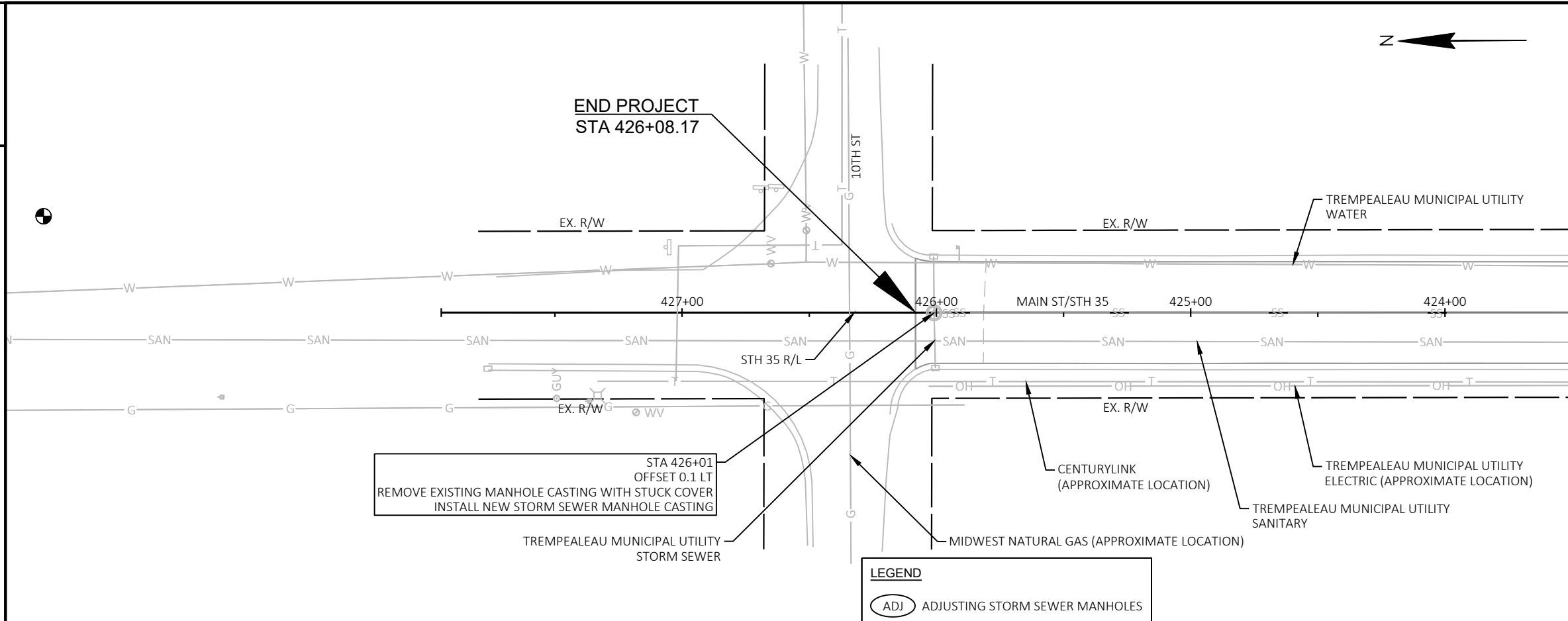
**PIPE BEDDING**

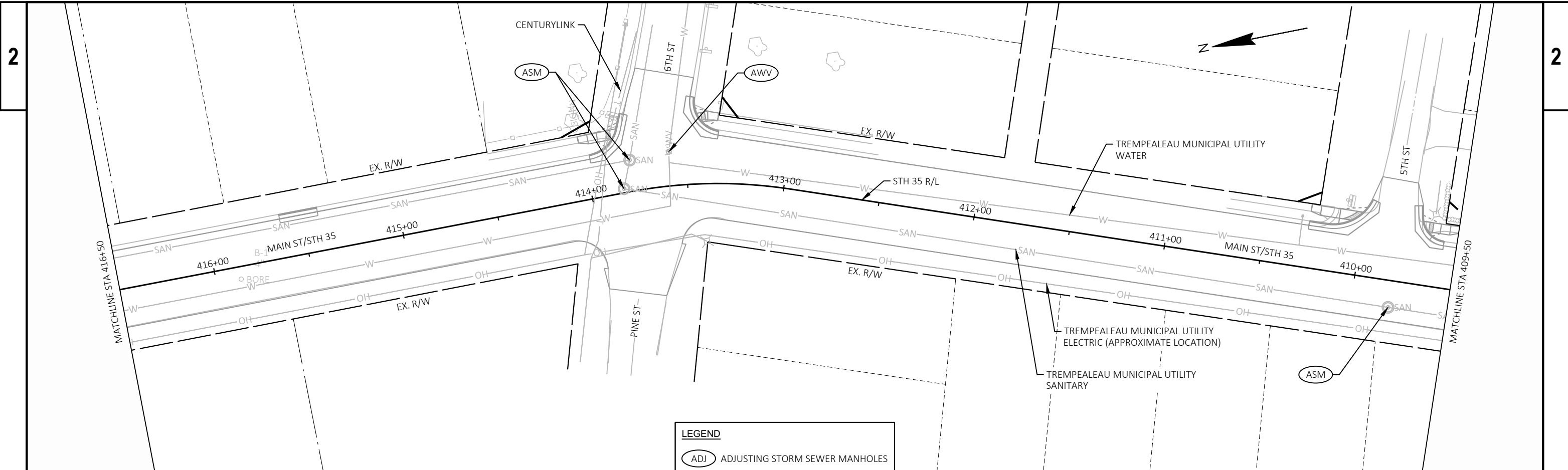


**GATE VALVE AND BOX**



**POLYETHYLENE PIPE WRAP FOR DIP**





**LEGEND**

- (ADJ) ADJUSTING STORM SEWER MANHOLES
- (ASM) ADJUSTING SANITARY MANHOLES
- (AWV) ADJUSTING WATER VALVES

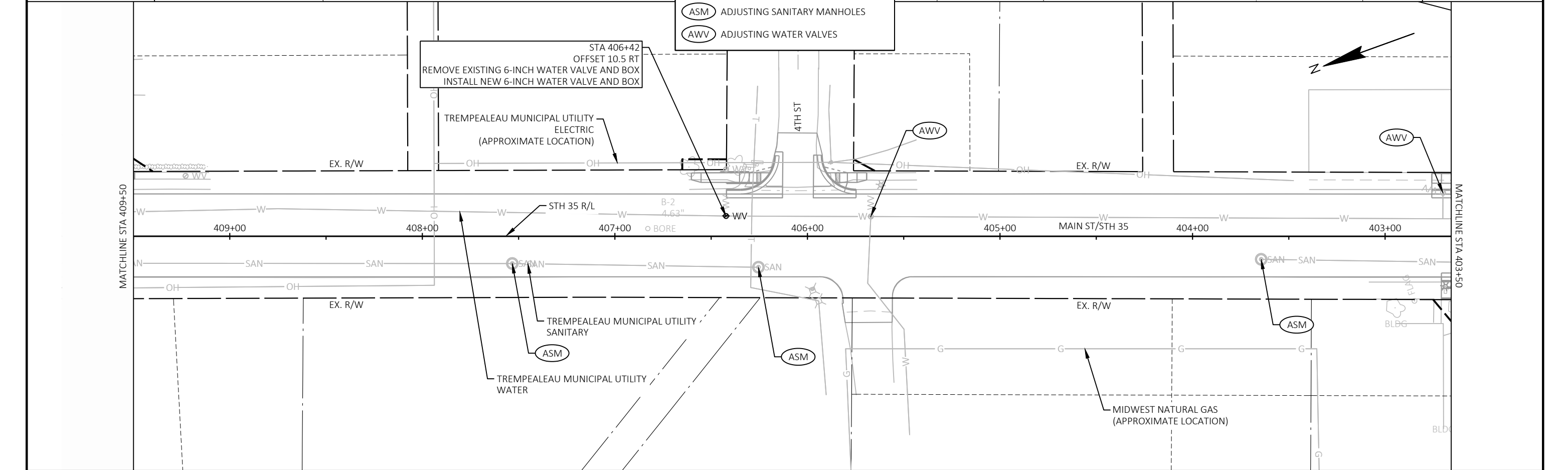
STA 406+42  
 OFFSET 10.5 RT  
 REMOVE EXISTING 6-INCH WATER VALVE AND BOX  
 INSTALL NEW 6-INCH WATER VALVE AND BOX

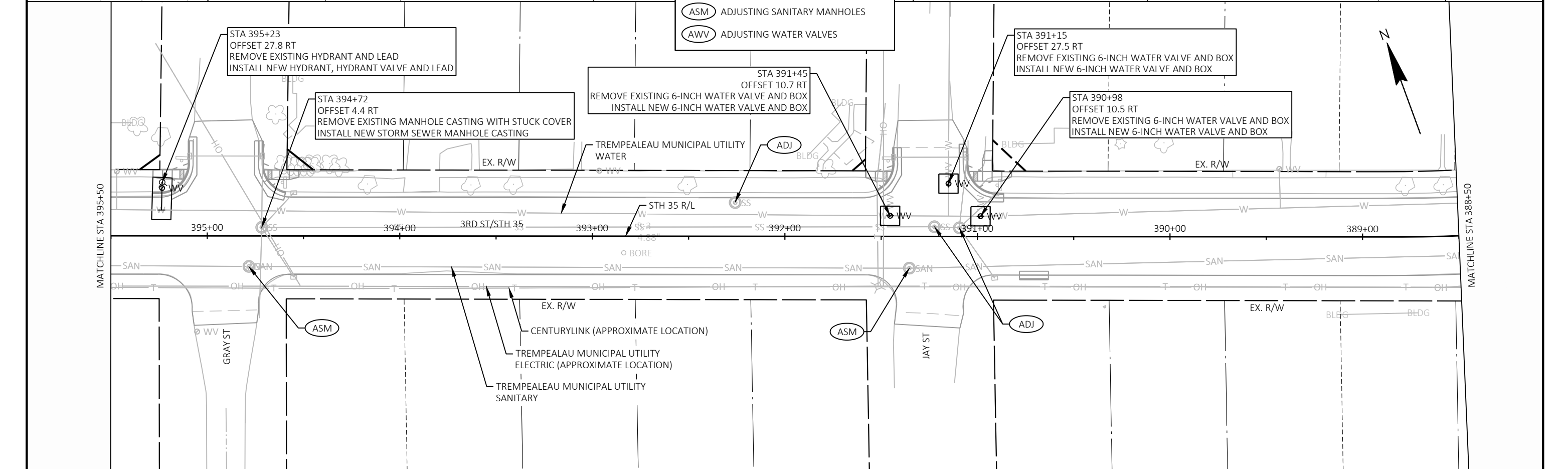
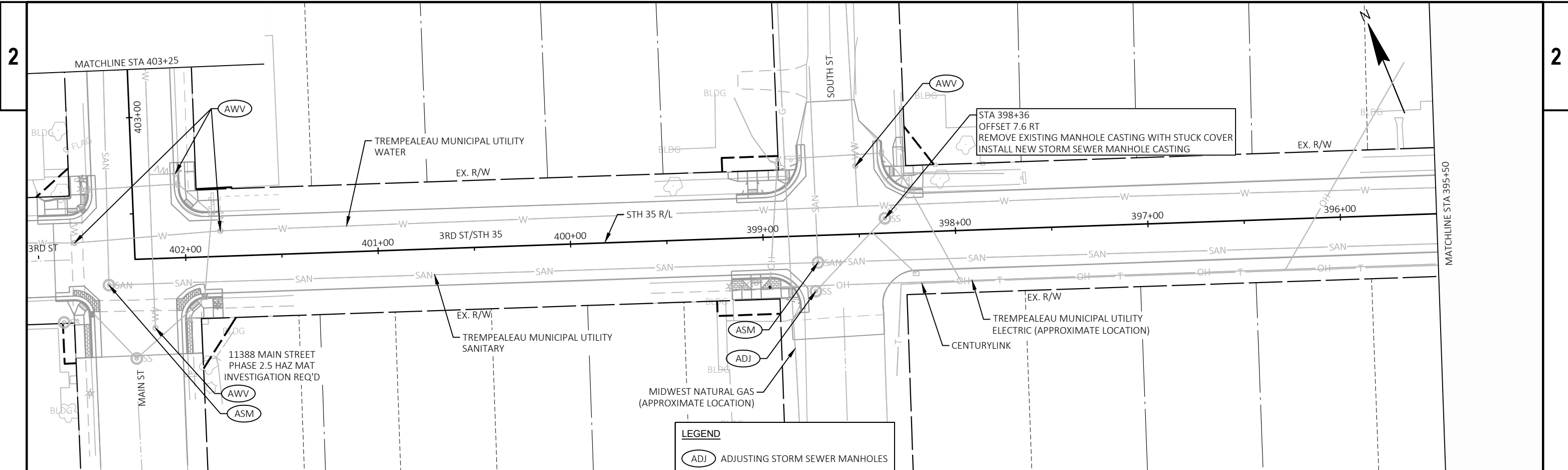
TREMPEALEAU MUNICIPAL UTILITY  
 ELECTRIC  
 (APPROXIMATE LOCATION)

TREMPEALEAU MUNICIPAL UTILITY  
 SANITARY  
 (ASM)

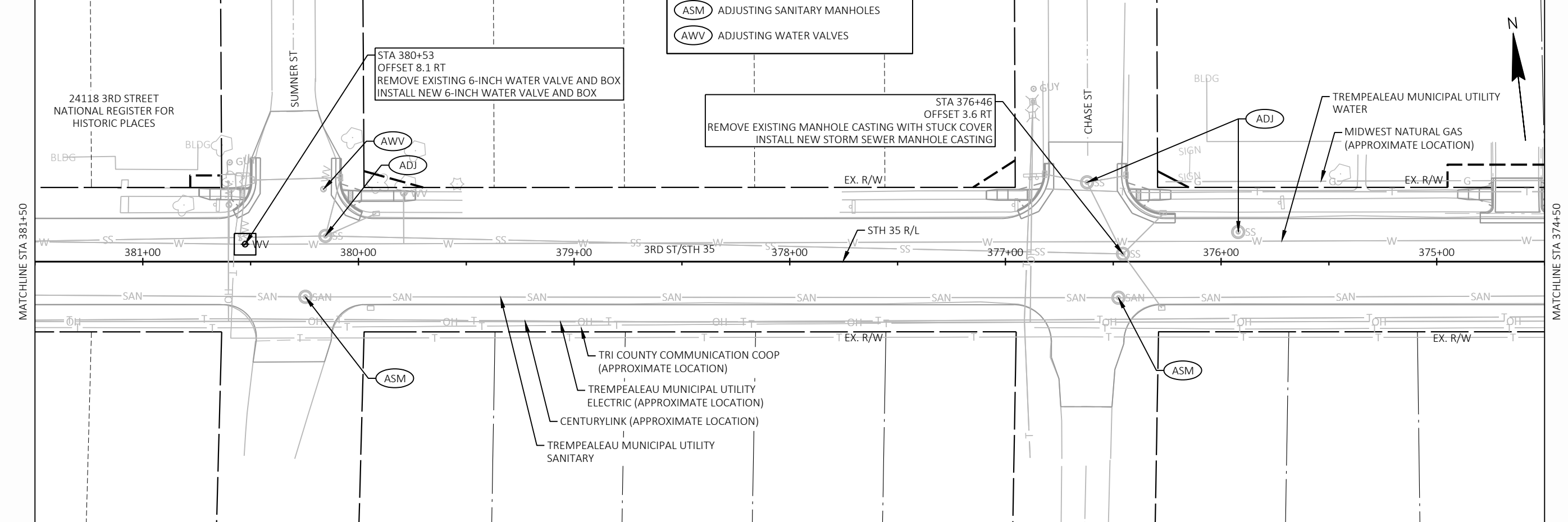
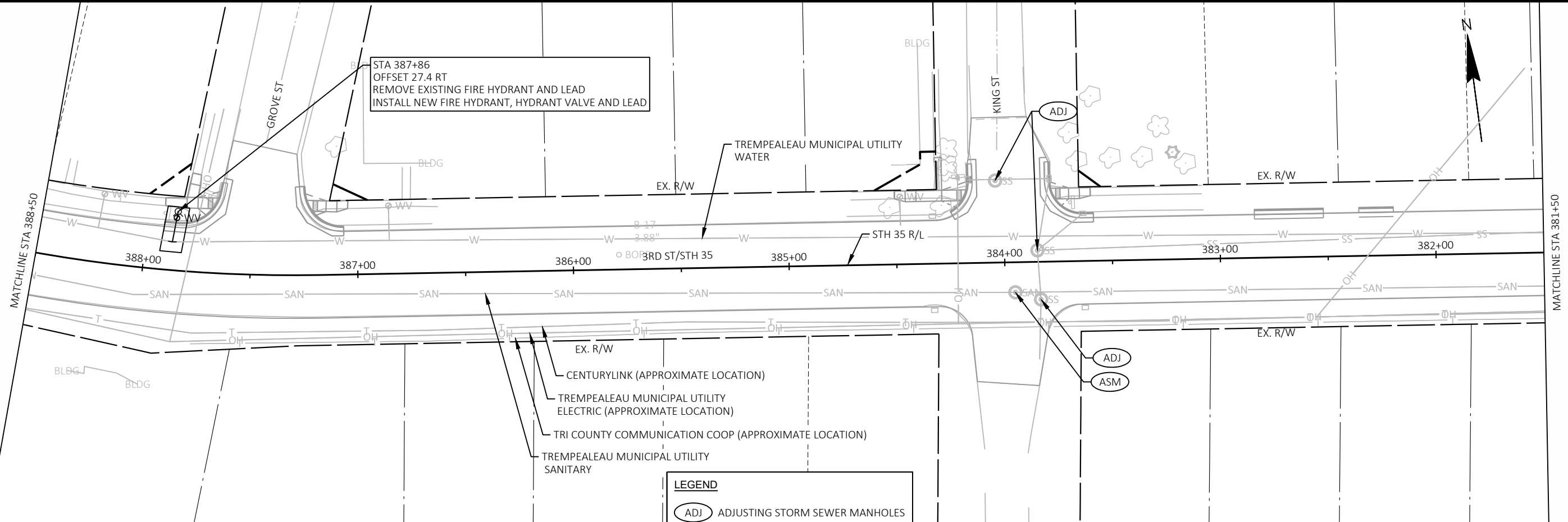
TREMPEALEAU MUNICIPAL UTILITY  
 WATER

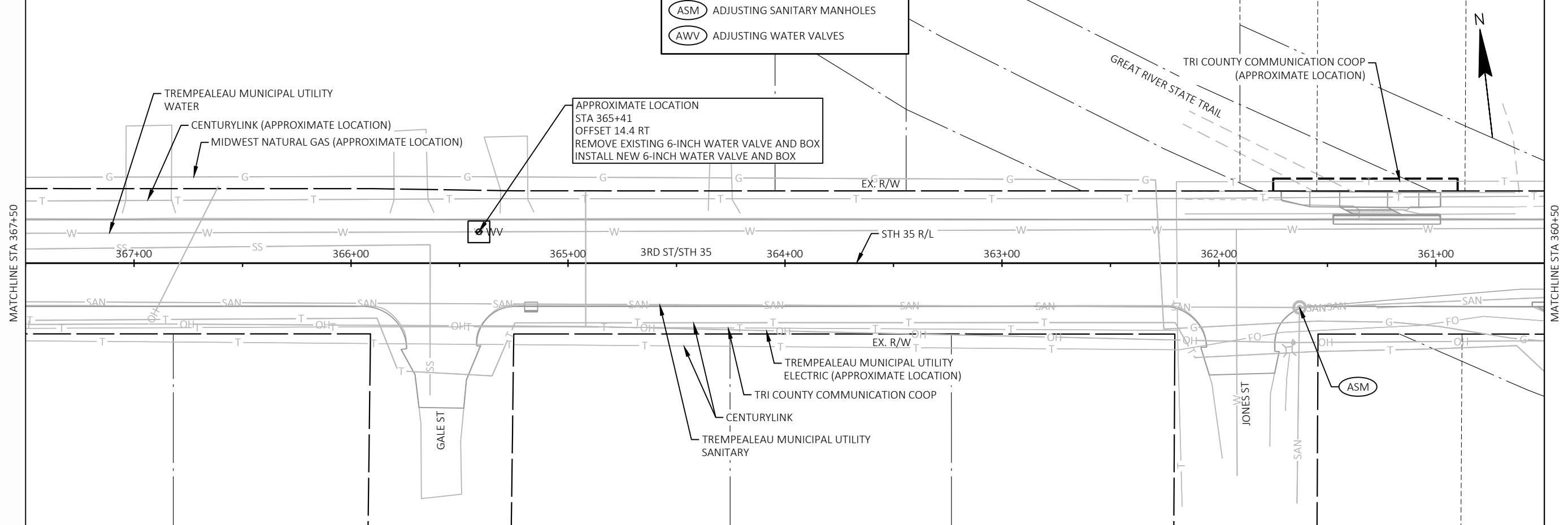
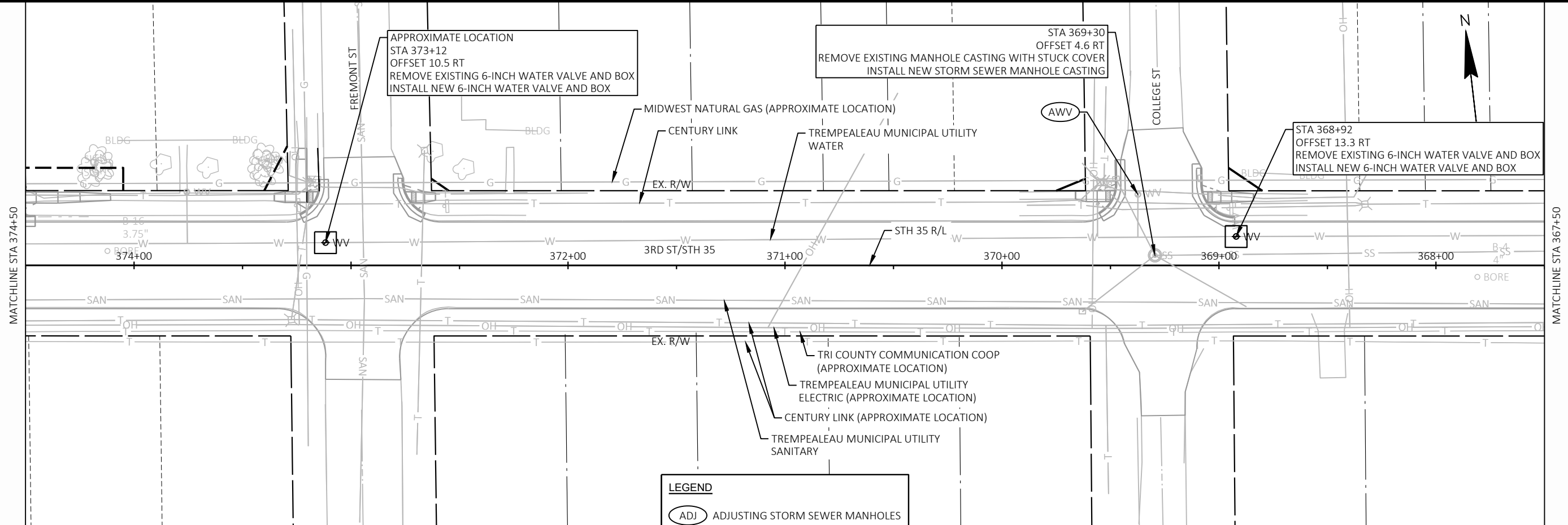
MIDWEST NATURAL GAS  
 (APPROXIMATE LOCATION)







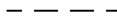




LIGHTING PLAN LEGEND

PROPOSED

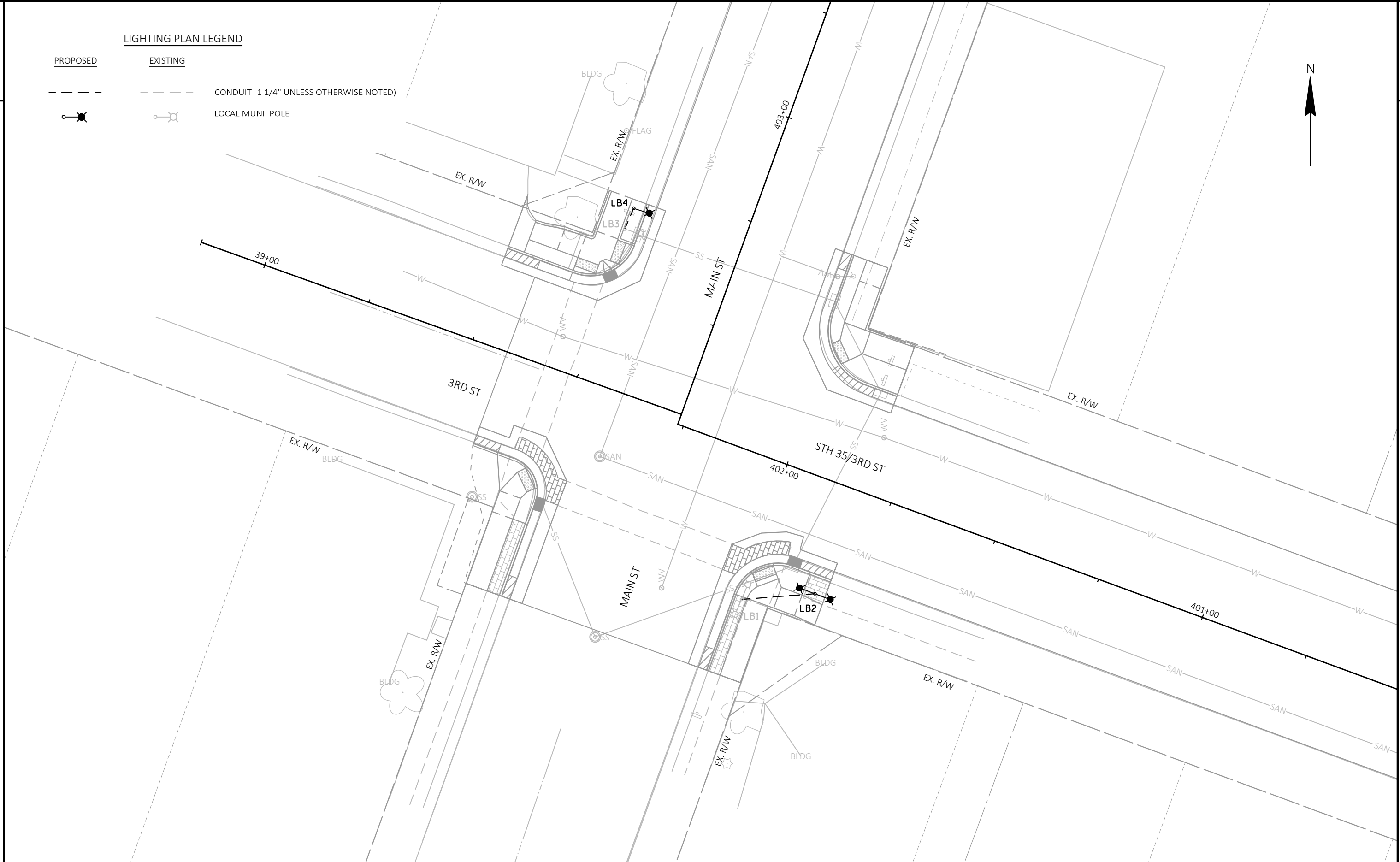
EXISTING



CONDUIT- 1 1/4" UNLESS OTHERWISE NOTED)

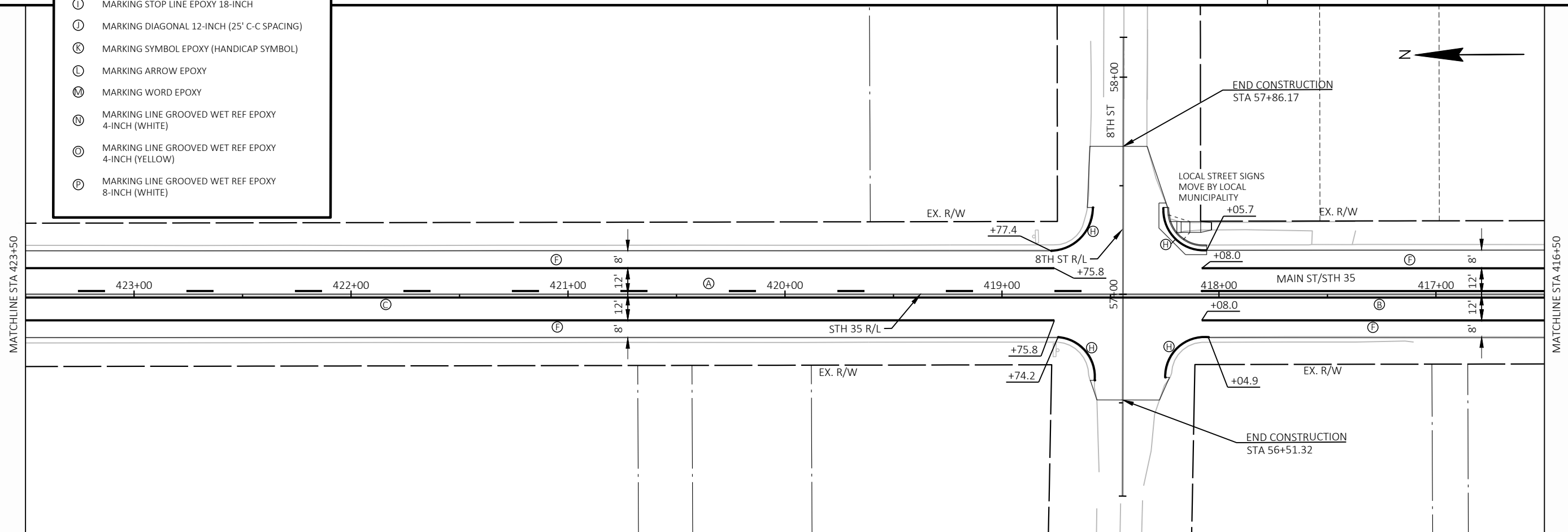
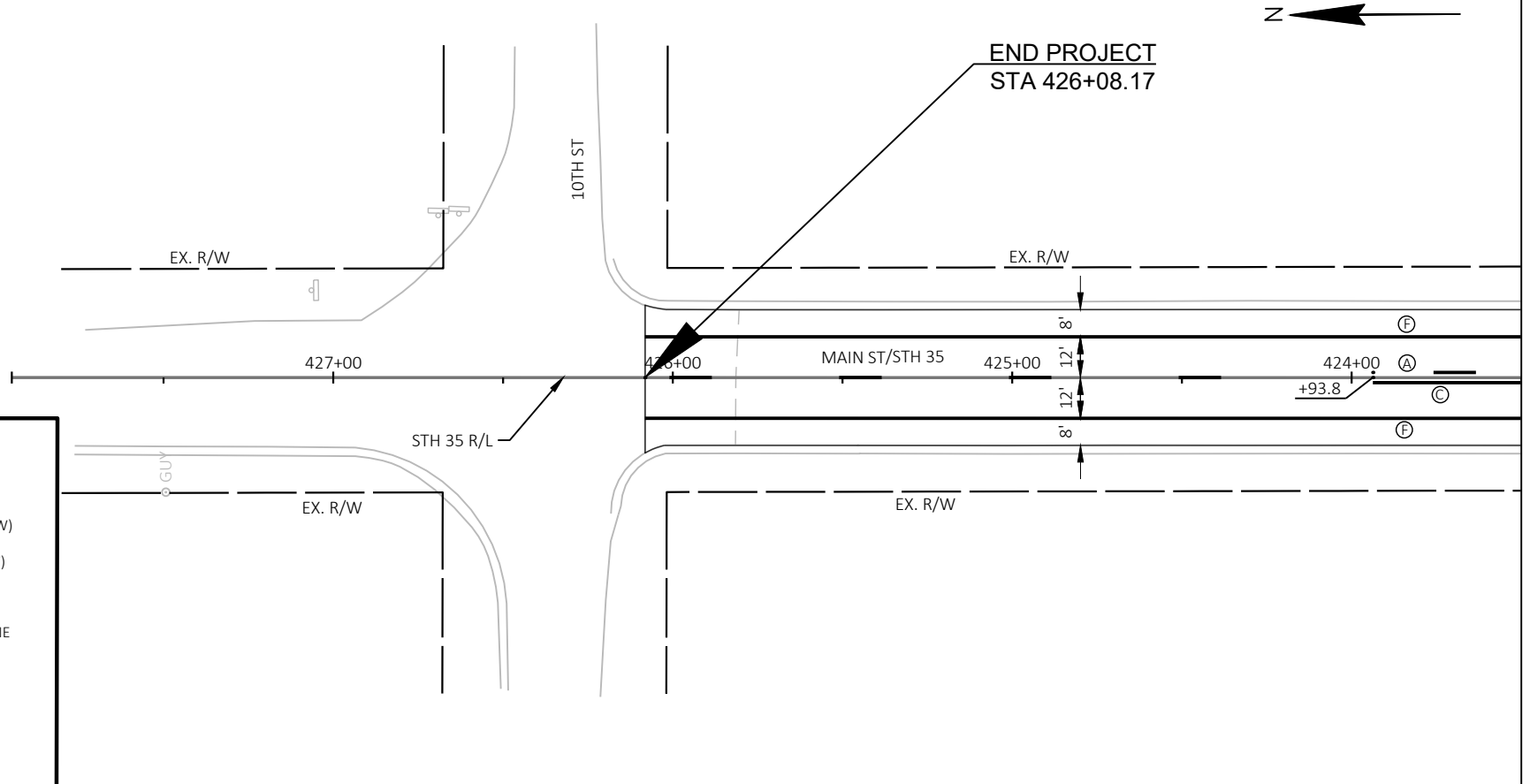


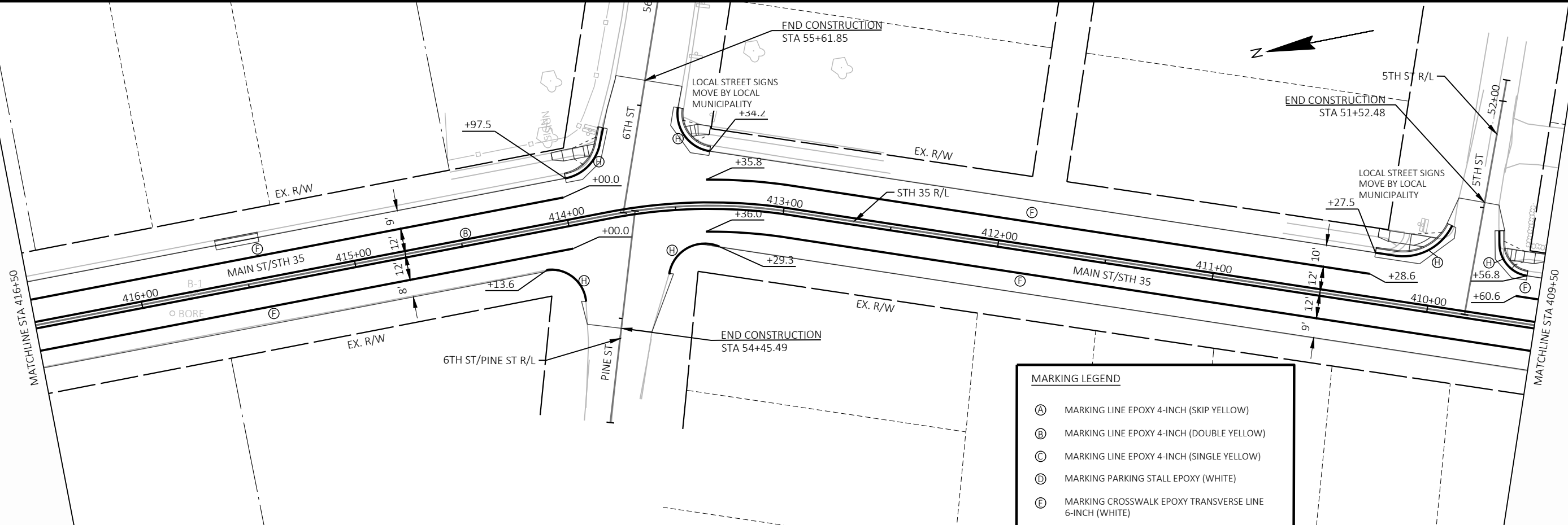
LOCAL MUNI. POLE



**MARKING LEGEND**

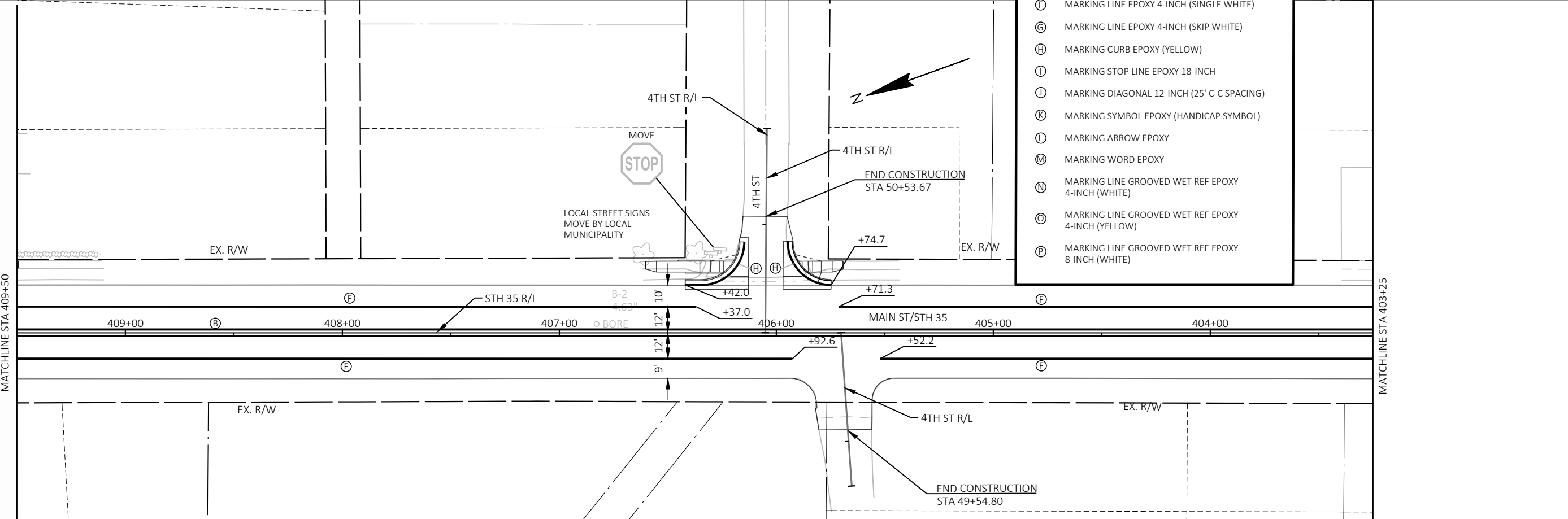
- (A) MARKING LINE EPOXY 4-INCH (SKIP YELLOW)
- (B) MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW)
- (C) MARKING LINE EPOXY 4-INCH (SINGLE YELLOW)
- (D) MARKING PARKING STALL EPOXY (WHITE)
- (E) MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)
- (F) MARKING LINE EPOXY 4-INCH (SINGLE WHITE)
- (G) MARKING LINE EPOXY 4-INCH (SKIP WHITE)
- (H) MARKING CURB EPOXY (YELLOW)
- (I) MARKING STOP LINE EPOXY 18-INCH
- (J) MARKING DIAGONAL 12-INCH (25' C-C SPACING)
- (K) MARKING SYMBOL EPOXY (HANDICAP SYMBOL)
- (L) MARKING ARROW EPOXY
- (M) MARKING WORD EPOXY
- (N) MARKING LINE GROOVED WET REF EPOXY 4-INCH (WHITE)
- (O) MARKING LINE GROOVED WET REF EPOXY 4-INCH (YELLOW)
- (P) MARKING LINE GROOVED WET REF EPOXY 8-INCH (WHITE)

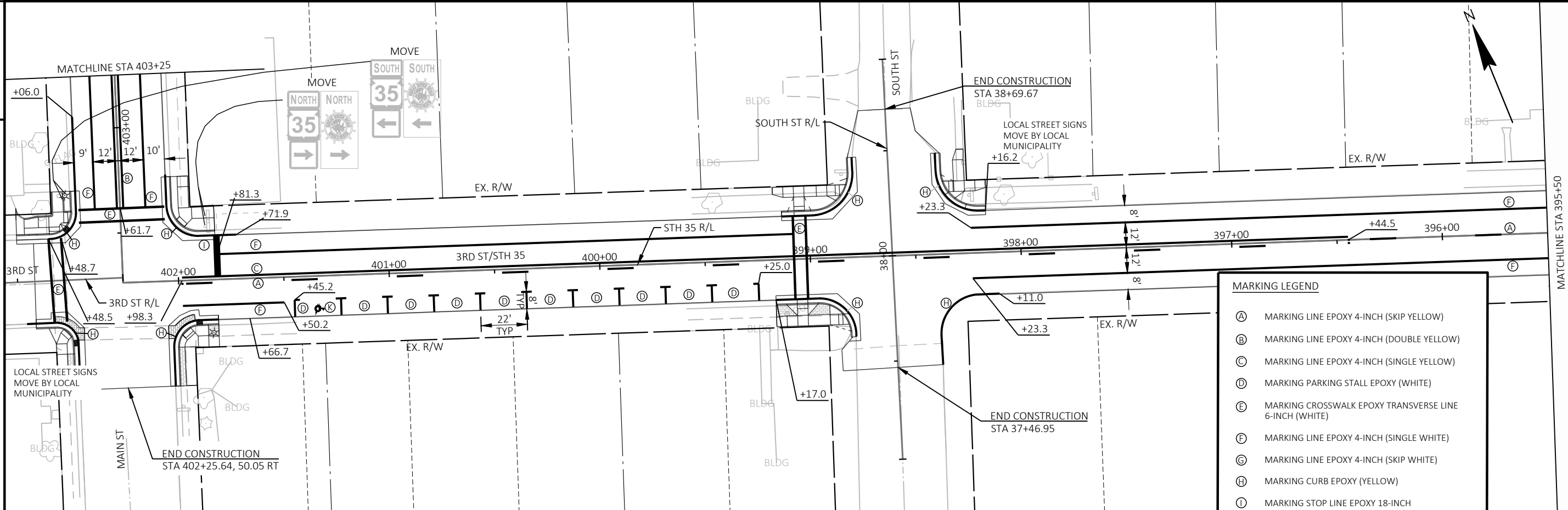




**MARKING LEGEND**

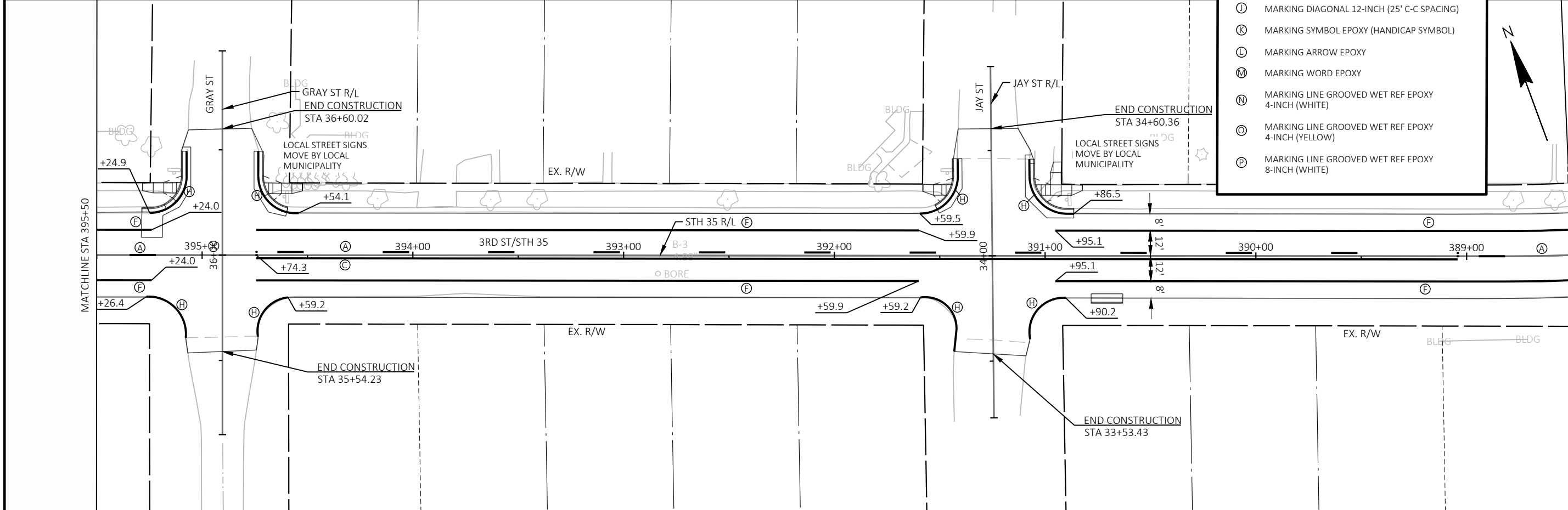
- Ⓐ MARKING LINE EPOXY 4-INCH (SKIP YELLOW)
- Ⓑ MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW)
- Ⓒ MARKING LINE EPOXY 4-INCH (SINGLE YELLOW)
- Ⓓ MARKING PARKING STALL EPOXY (WHITE)
- Ⓔ MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)
- Ⓕ MARKING LINE EPOXY 4-INCH (SINGLE WHITE)
- Ⓖ MARKING LINE EPOXY 4-INCH (SKIP WHITE)
- Ⓗ MARKING CURB EPOXY (YELLOW)
- Ⓘ MARKING STOP LINE EPOXY 18-INCH
- Ⓚ MARKING DIAGONAL 12-INCH (25' C-C SPACING)
- Ⓛ MARKING SYMBOL EPOXY (HANDICAP SYMBOL)
- Ⓜ MARKING ARROW EPOXY
- Ⓝ MARKING WORD EPOXY
- Ⓝ MARKING LINE GROOVED WET REF EPOXY 4-INCH (WHITE)
- Ⓝ MARKING LINE GROOVED WET REF EPOXY 4-INCH (YELLOW)
- Ⓝ MARKING LINE GROOVED WET REF EPOXY 8-INCH (WHITE)

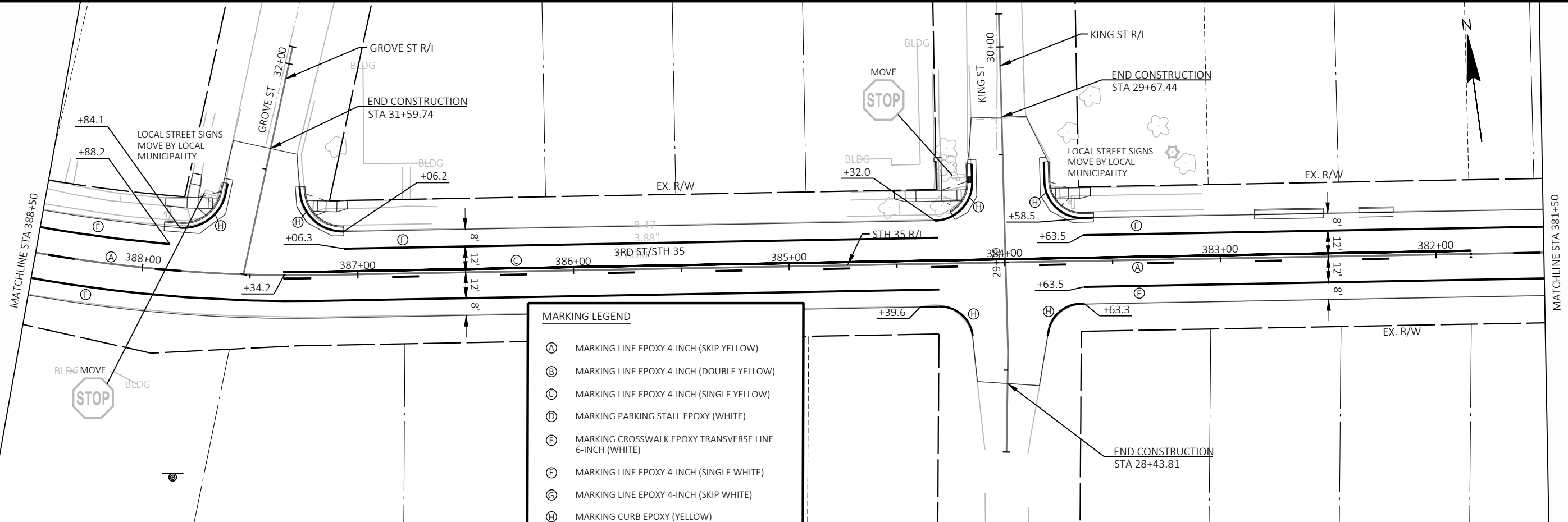




**MARKING LEGEND**

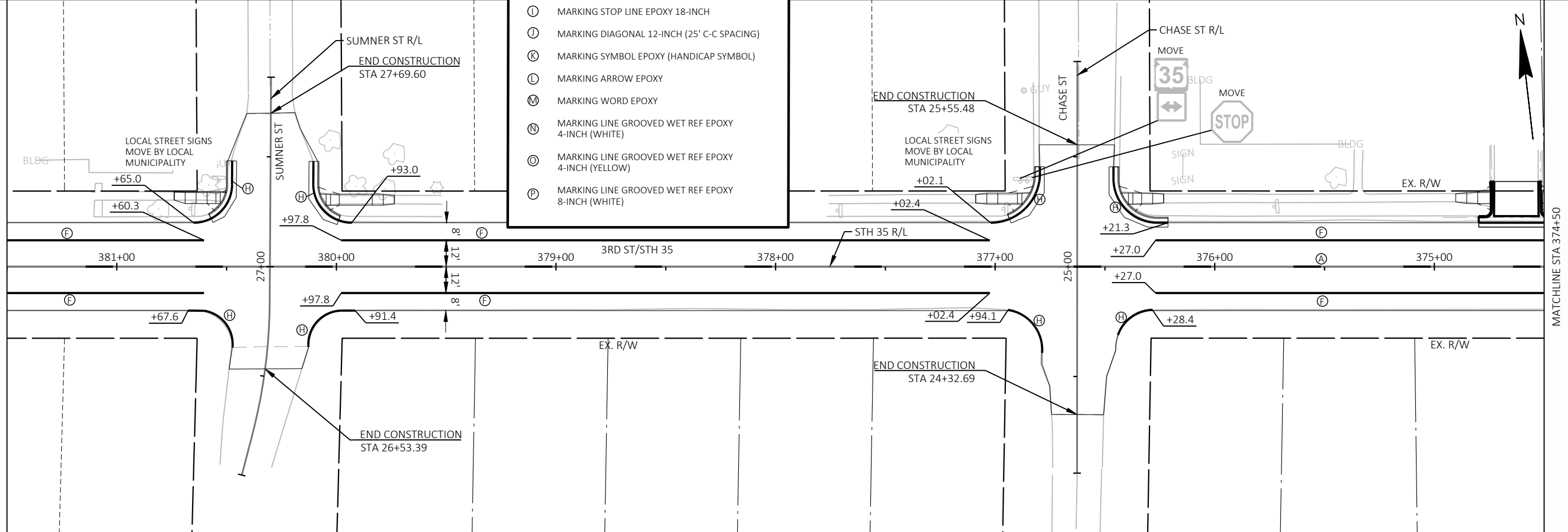
- (A) MARKING LINE EPOXY 4-INCH (SKIP YELLOW)
- (B) MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW)
- (C) MARKING LINE EPOXY 4-INCH (SINGLE YELLOW)
- (D) MARKING PARKING STALL EPOXY (WHITE)
- (E) MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)
- (F) MARKING LINE EPOXY 4-INCH (SINGLE WHITE)
- (G) MARKING LINE EPOXY 4-INCH (SKIP WHITE)
- (H) MARKING CURB EPOXY (YELLOW)
- (I) MARKING STOP LINE EPOXY 18-INCH
- (J) MARKING DIAGONAL 12-INCH (25' C-C SPACING)
- (K) MARKING SYMBOL EPOXY (HANDICAP SYMBOL)
- (L) MARKING ARROW EPOXY
- (M) MARKING WORD EPOXY
- (N) MARKING LINE GROOVED WET REF EPOXY 4-INCH (WHITE)
- (O) MARKING LINE GROOVED WET REF EPOXY 4-INCH (YELLOW)
- (P) MARKING LINE GROOVED WET REF EPOXY 8-INCH (WHITE)

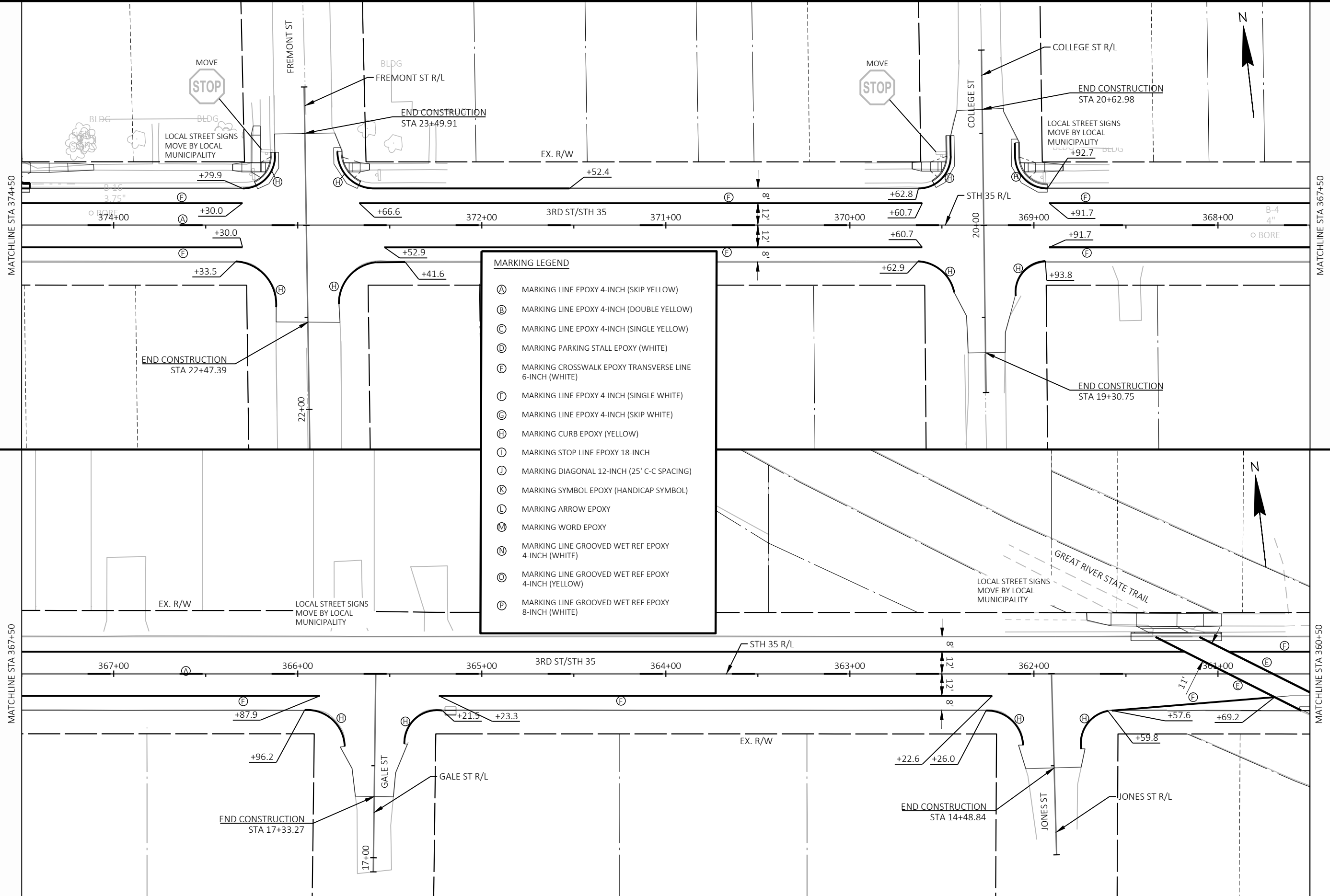




**MARKING LEGEND**

- (A) MARKING LINE EPOXY 4-INCH (SKIP YELLOW)
- (B) MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW)
- (C) MARKING LINE EPOXY 4-INCH (SINGLE YELLOW)
- (D) MARKING PARKING STALL EPOXY (WHITE)
- (E) MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)
- (F) MARKING LINE EPOXY 4-INCH (SINGLE WHITE)
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- (M) MARKING WORD EPOXY
- (N) MARKING LINE GROOVED WET REF EPOXY 4-INCH (WHITE)
- (O) MARKING LINE GROOVED WET REF EPOXY 4-INCH (YELLOW)
- (P) MARKING LINE GROOVED WET REF EPOXY 8-INCH (WHITE)

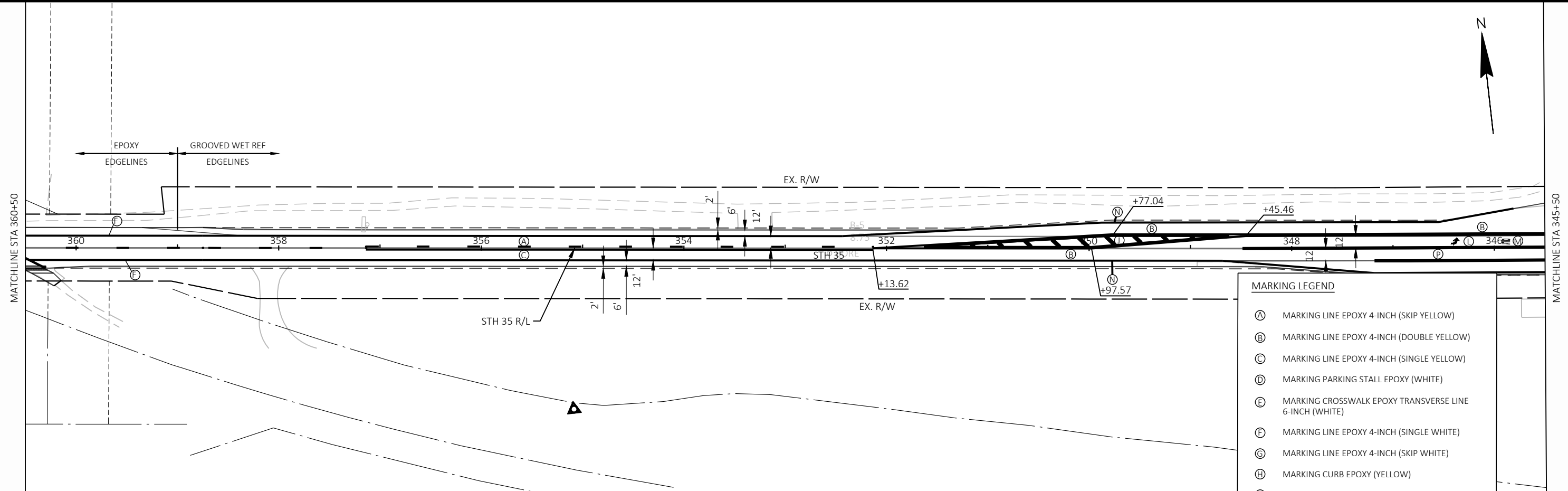




**MARKING LEGEND**

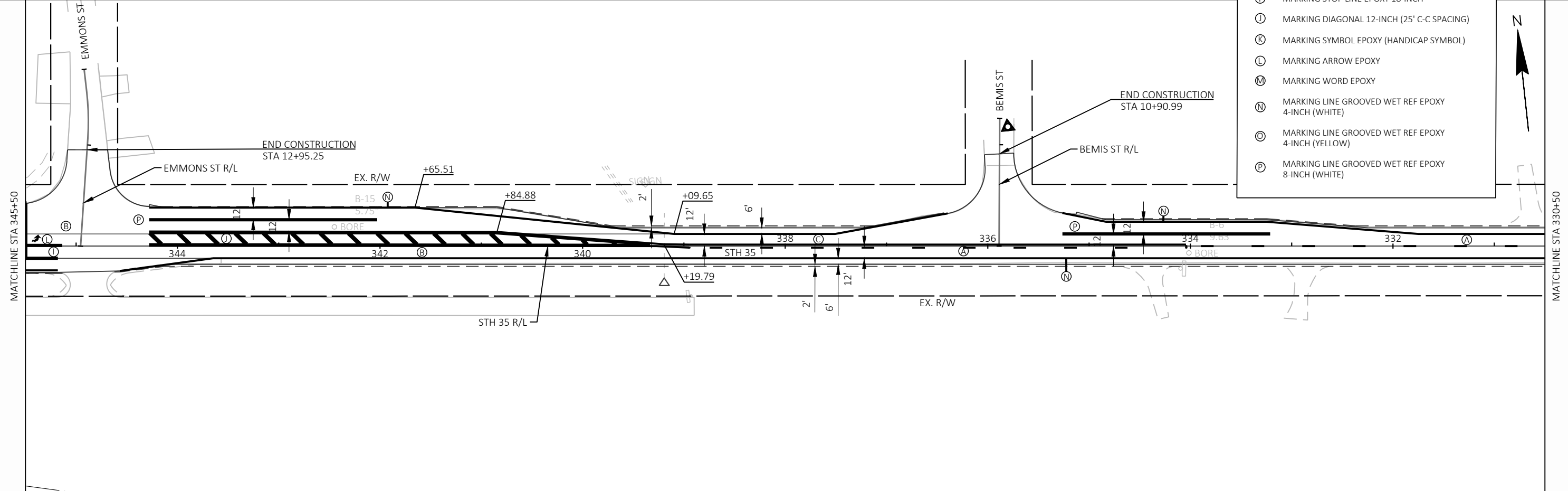
- (A) MARKING LINE EPOXY 4-INCH (SKIP YELLOW)
- (B) MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW)
- (C) MARKING LINE EPOXY 4-INCH (SINGLE YELLOW)
- (D) MARKING PARKING STALL EPOXY (WHITE)
- (E) MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)
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- (G) MARKING LINE EPOXY 4-INCH (SKIP WHITE)
- (H) MARKING CURB EPOXY (YELLOW)
- (I) MARKING STOP LINE EPOXY 18-INCH
- (J) MARKING DIAGONAL 12-INCH (25' C-C SPACING)
- (K) MARKING SYMBOL EPOXY (HANDICAP SYMBOL)
- (L) MARKING ARROW EPOXY
- (M) MARKING WORD EPOXY
- (N) MARKING LINE GROOVED WET REF EPOXY 4-INCH (WHITE)
- (O) MARKING LINE GROOVED WET REF EPOXY 4-INCH (YELLOW)
- (P) MARKING LINE GROOVED WET REF EPOXY 8-INCH (WHITE)

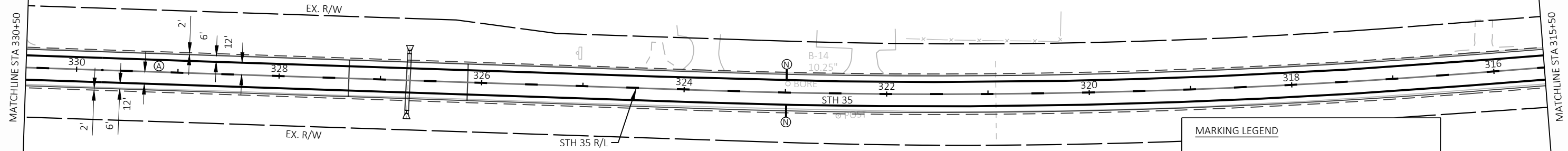




**MARKING LEGEND**

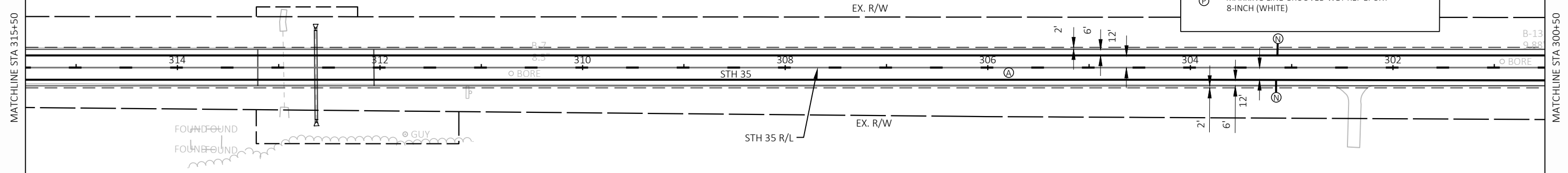
- (A) MARKING LINE EPOXY 4-INCH (SKIP YELLOW)
- (B) MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW)
- (C) MARKING LINE EPOXY 4-INCH (SINGLE YELLOW)
- (D) MARKING PARKING STALL EPOXY (WHITE)
- (E) MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)
- (F) MARKING LINE EPOXY 4-INCH (SINGLE WHITE)
- (G) MARKING LINE EPOXY 4-INCH (SKIP WHITE)
- (H) MARKING CURB EPOXY (YELLOW)
- (I) MARKING STOP LINE EPOXY 18-INCH
- (J) MARKING DIAGONAL 12-INCH (25' C-C SPACING)
- (K) MARKING SYMBOL EPOXY (HANDICAP SYMBOL)
- (L) MARKING ARROW EPOXY
- (M) MARKING WORD EPOXY
- (N) MARKING LINE GROOVED WET REF EPOXY 4-INCH (WHITE)
- (O) MARKING LINE GROOVED WET REF EPOXY 4-INCH (YELLOW)
- (P) MARKING LINE GROOVED WET REF EPOXY 8-INCH (WHITE)





**MARKING LEGEND**

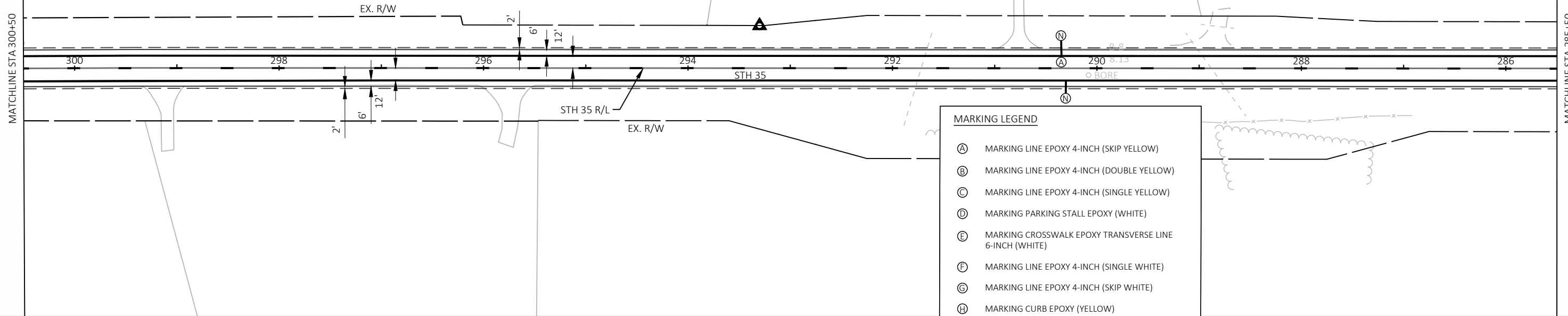
- (A) MARKING LINE EPOXY 4-INCH (SKIP YELLOW)
- (B) MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW)
- (C) MARKING LINE EPOXY 4-INCH (SINGLE YELLOW)
- (D) MARKING PARKING STALL EPOXY (WHITE)
- (E) MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)
- (F) MARKING LINE EPOXY 4-INCH (SINGLE WHITE)
- (G) MARKING LINE EPOXY 4-INCH (SKIP WHITE)
- (H) MARKING CURB EPOXY (YELLOW)
- (I) MARKING STOP LINE EPOXY 18-INCH
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- (N) MARKING LINE GROOVED WET REF EPOXY 4-INCH (WHITE)
- (O) MARKING LINE GROOVED WET REF EPOXY 4-INCH (YELLOW)
- (P) MARKING LINE GROOVED WET REF EPOXY 8-INCH (WHITE)





MATCHLINE STA 300+50

MATCHLINE STA 285+50



**MARKING LEGEND**

- (A) MARKING LINE EPOXY 4-INCH (SKIP YELLOW)
- (B) MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW)
- (C) MARKING LINE EPOXY 4-INCH (SINGLE YELLOW)
- (D) MARKING PARKING STALL EPOXY (WHITE)
- (E) MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)
- (F) MARKING LINE EPOXY 4-INCH (SINGLE WHITE)
- (G) MARKING LINE EPOXY 4-INCH (SKIP WHITE)
- (H) MARKING CURB EPOXY (YELLOW)
- (I) MARKING STOP LINE EPOXY 18-INCH
- (J) MARKING DIAGONAL 12-INCH (25' C-C SPACING)
- (K) MARKING SYMBOL EPOXY (HANDICAP SYMBOL)
- (L) MARKING ARROW EPOXY
- (M) MARKING WORD EPOXY
- (N) MARKING LINE GROOVED WET REF EPOXY 4-INCH (WHITE)
- (O) MARKING LINE GROOVED WET REF EPOXY 4-INCH (YELLOW)
- (P) MARKING LINE GROOVED WET REF EPOXY 8-INCH (WHITE)

MATCHLINE STA 285+50

MATCHLINE STA 270+50





MATCHLINE STA 270+50

MATCHLINE STA 255+50

MATCHLINE STA 255+50

MATCHLINE STA 240+50

EX. R/W

STH 35 R/L

STH 35

B-11  
7.75"

**MARKING LEGEND**

- (A) MARKING LINE EPOXY 4-INCH (SKIP YELLOW)
- (B) MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW)
- (C) MARKING LINE EPOXY 4-INCH (SINGLE YELLOW)
- (D) MARKING PARKING STALL EPOXY (WHITE)
- (E) MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)
- (F) MARKING LINE EPOXY 4-INCH (SINGLE WHITE)
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- (H) MARKING CURB EPOXY (YELLOW)
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- (K) MARKING SYMBOL EPOXY (HANDICAP SYMBOL)
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- (N) MARKING LINE GROOVED WET REF EPOXY 4-INCH (WHITE)
- (O) MARKING LINE GROOVED WET REF EPOXY 4-INCH (YELLOW)
- (P) MARKING LINE GROOVED WET REF EPOXY 8-INCH (WHITE)

EX. R/W

EX. R/W

EX. R/W

SOUTH ST

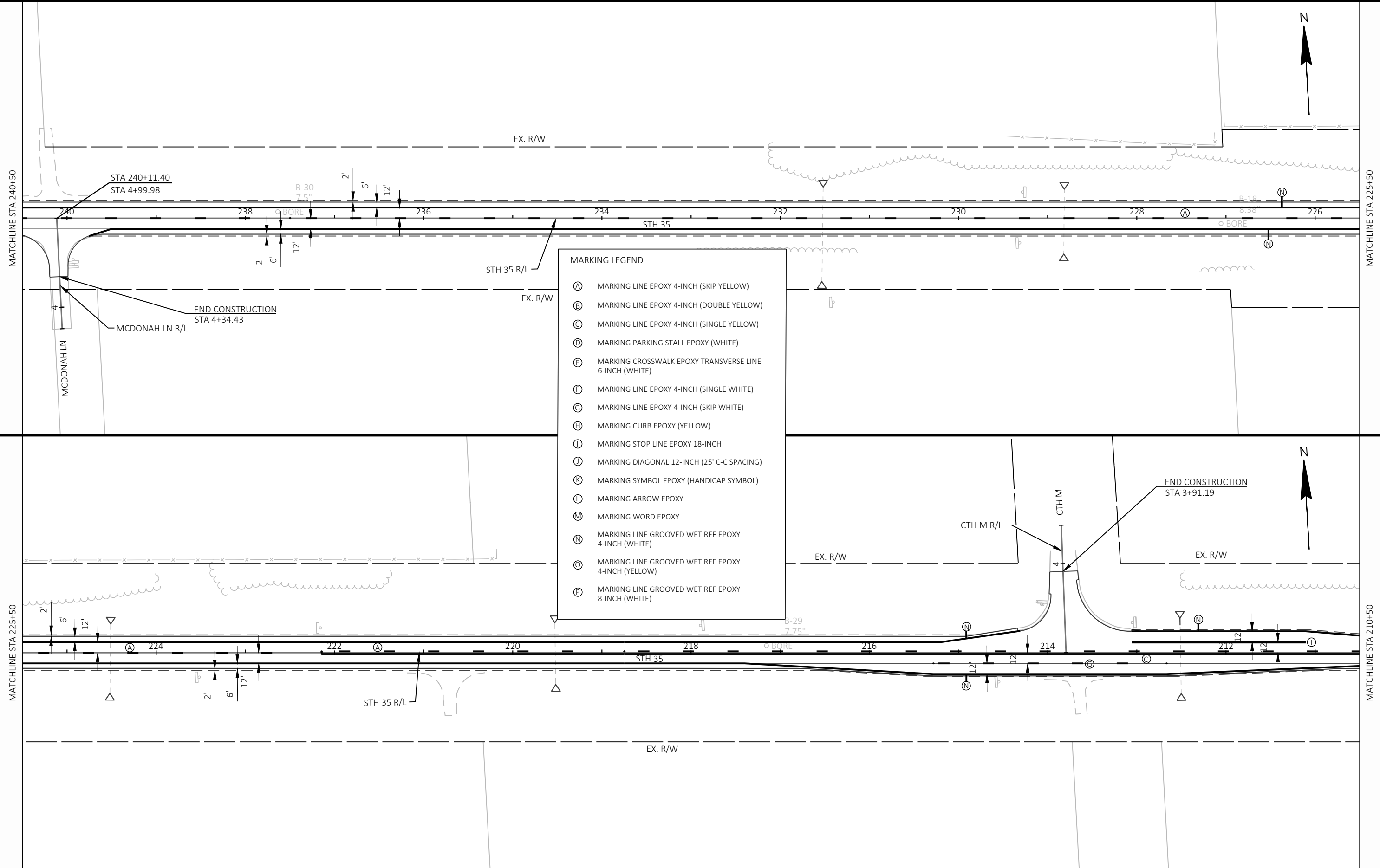
END CONSTRUCTION  
STA 6+75.22

SOUTH ST R/L

MATCHLINE STA 255+50

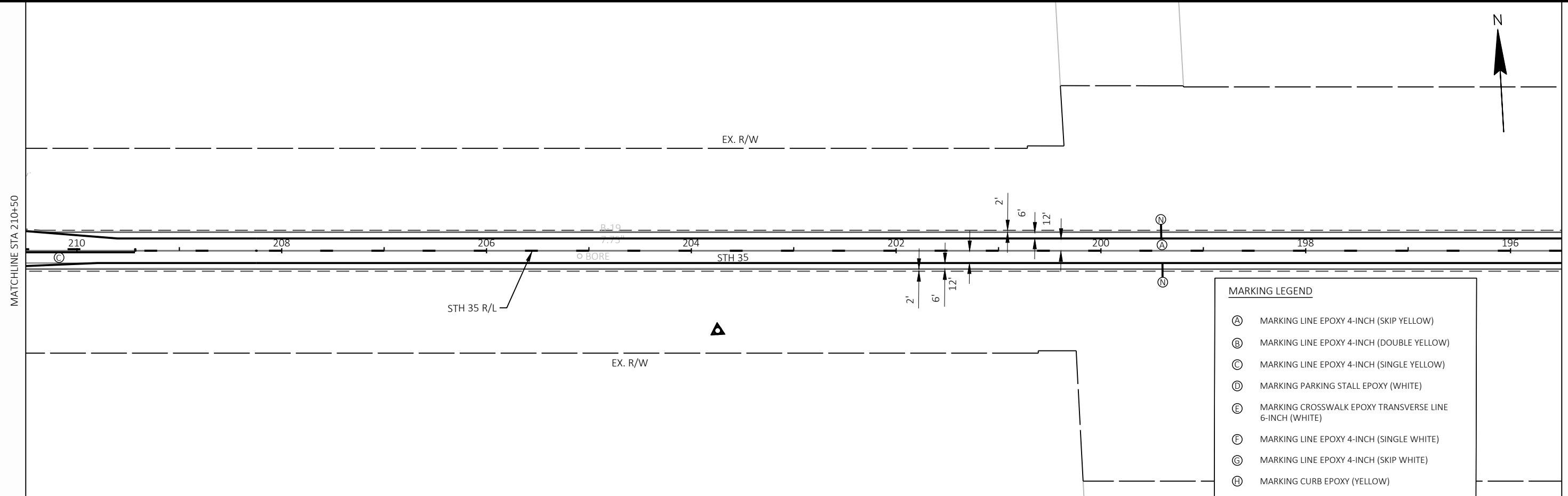
MATCHLINE STA 240+50





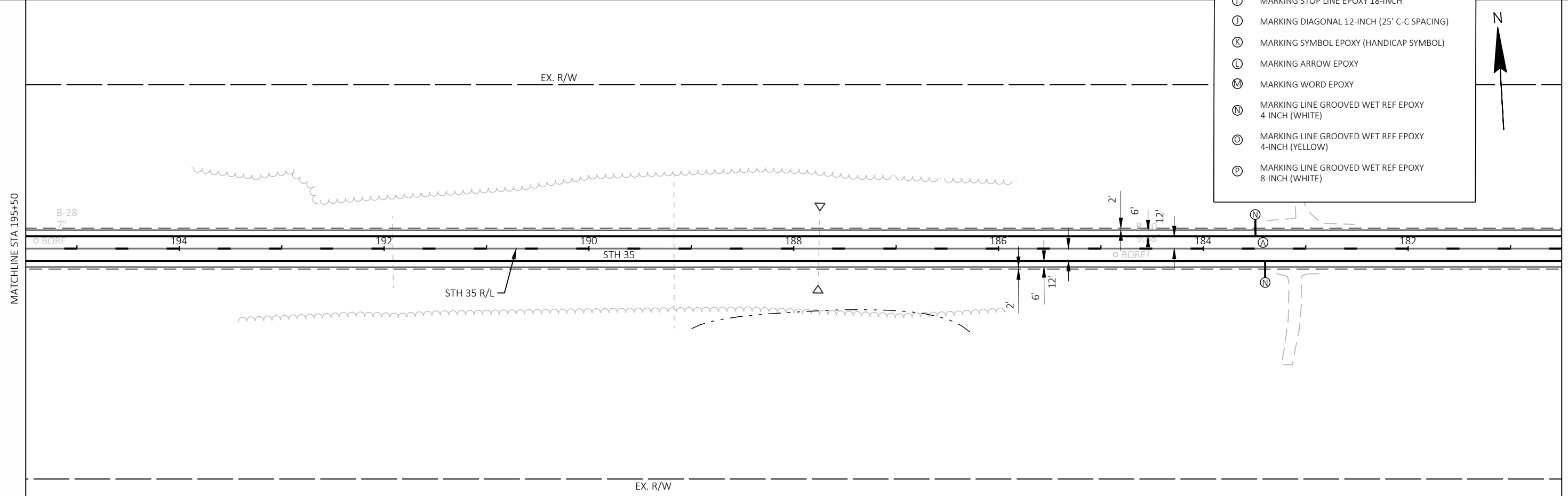
**MARKING LEGEND**

- (A) MARKING LINE EPOXY 4-INCH (SKIP YELLOW)
- (B) MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW)
- (C) MARKING LINE EPOXY 4-INCH (SINGLE YELLOW)
- (D) MARKING PARKING STALL EPOXY (WHITE)
- (E) MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)
- (F) MARKING LINE EPOXY 4-INCH (SINGLE WHITE)
- (G) MARKING LINE EPOXY 4-INCH (SKIP WHITE)
- (H) MARKING CURB EPOXY (YELLOW)
- (I) MARKING STOP LINE EPOXY 18-INCH
- (J) MARKING DIAGONAL 12-INCH (25' C-C SPACING)
- (K) MARKING SYMBOL EPOXY (HANDICAP SYMBOL)
- (L) MARKING ARROW EPOXY
- (M) MARKING WORD EPOXY
- (N) MARKING LINE GROOVED WET REF EPOXY 4-INCH (WHITE)
- (O) MARKING LINE GROOVED WET REF EPOXY 4-INCH (YELLOW)
- (P) MARKING LINE GROOVED WET REF EPOXY 8-INCH (WHITE)



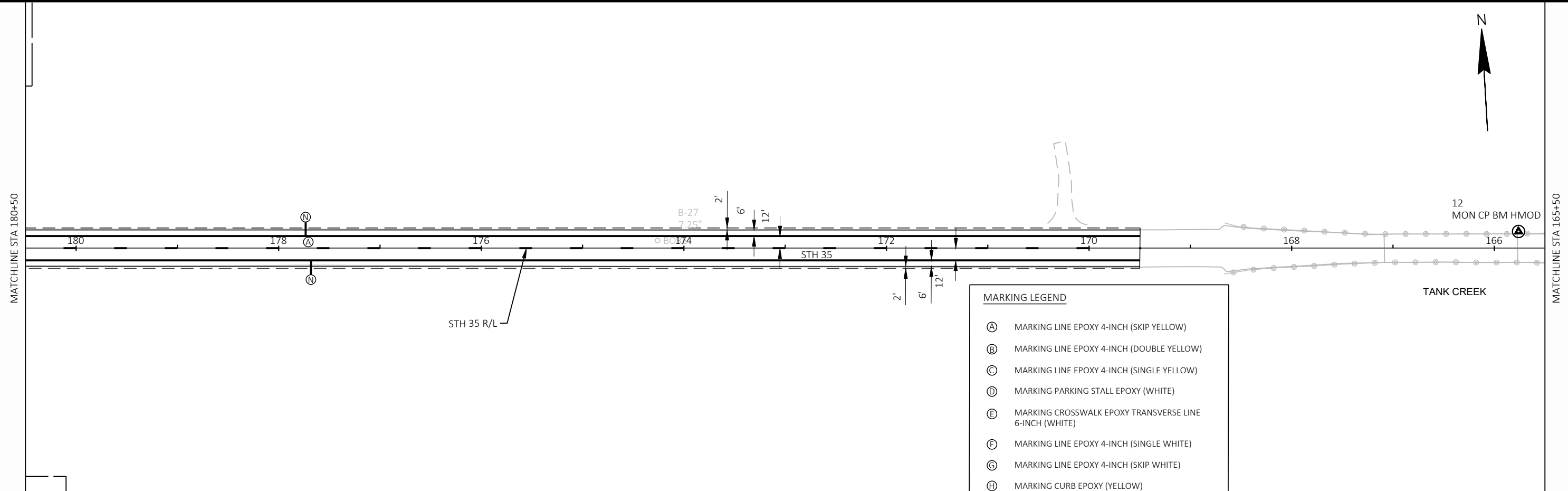
**MARKING LEGEND**

- (A) MARKING LINE EPOXY 4-INCH (SKIP YELLOW)
- (B) MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW)
- (C) MARKING LINE EPOXY 4-INCH (SINGLE YELLOW)
- (D) MARKING PARKING STALL EPOXY (WHITE)
- (E) MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)
- (F) MARKING LINE EPOXY 4-INCH (SINGLE WHITE)
- (G) MARKING LINE EPOXY 4-INCH (SKIP WHITE)
- (H) MARKING CURB EPOXY (YELLOW)
- (I) MARKING STOP LINE EPOXY 18-INCH
- (J) MARKING DIAGONAL 12-INCH (25' C-C SPACING)
- (K) MARKING SYMBOL EPOXY (HANDICAP SYMBOL)
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- (M) MARKING WORD EPOXY
- (N) MARKING LINE GROOVED WET REF EPOXY 4-INCH (WHITE)
- (O) MARKING LINE GROOVED WET REF EPOXY 4-INCH (YELLOW)
- (P) MARKING LINE GROOVED WET REF EPOXY 8-INCH (WHITE)



MATCHLINE STA 180+50

MATCHLINE STA 165+50

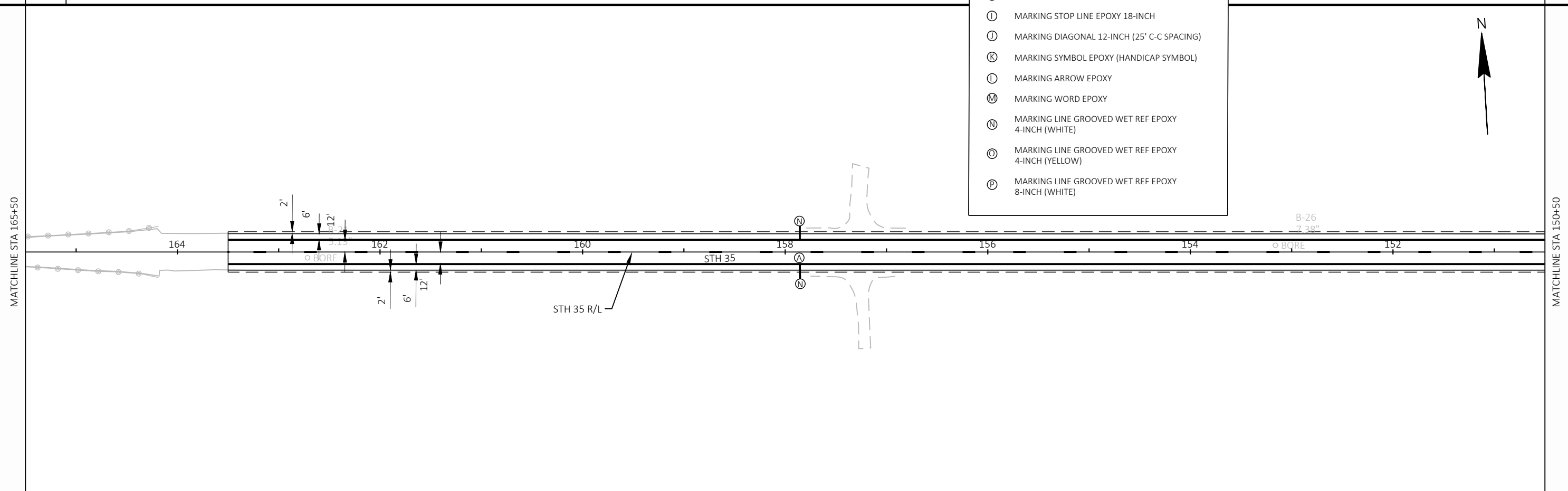


**MARKING LEGEND**

- (A) MARKING LINE EPOXY 4-INCH (SKIP YELLOW)
- (B) MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW)
- (C) MARKING LINE EPOXY 4-INCH (SINGLE YELLOW)
- (D) MARKING PARKING STALL EPOXY (WHITE)
- (E) MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)
- (F) MARKING LINE EPOXY 4-INCH (SINGLE WHITE)
- (G) MARKING LINE EPOXY 4-INCH (SKIP WHITE)
- (H) MARKING CURB EPOXY (YELLOW)
- (I) MARKING STOP LINE EPOXY 18-INCH
- (J) MARKING DIAGONAL 12-INCH (25' C-C SPACING)
- (K) MARKING SYMBOL EPOXY (HANDICAP SYMBOL)
- (L) MARKING ARROW EPOXY
- (M) MARKING WORD EPOXY
- (N) MARKING LINE GROOVED WET REF EPOXY 4-INCH (WHITE)
- (O) MARKING LINE GROOVED WET REF EPOXY 4-INCH (YELLOW)
- (P) MARKING LINE GROOVED WET REF EPOXY 8-INCH (WHITE)

MATCHLINE STA 165+50

MATCHLINE STA 150+50



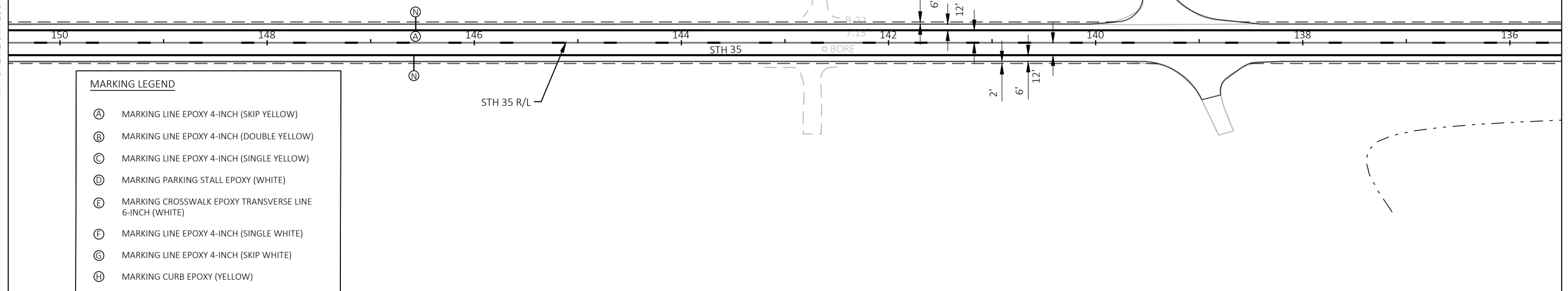
MATCHLINE STA 150+50

MATCHLINE STA 135+50



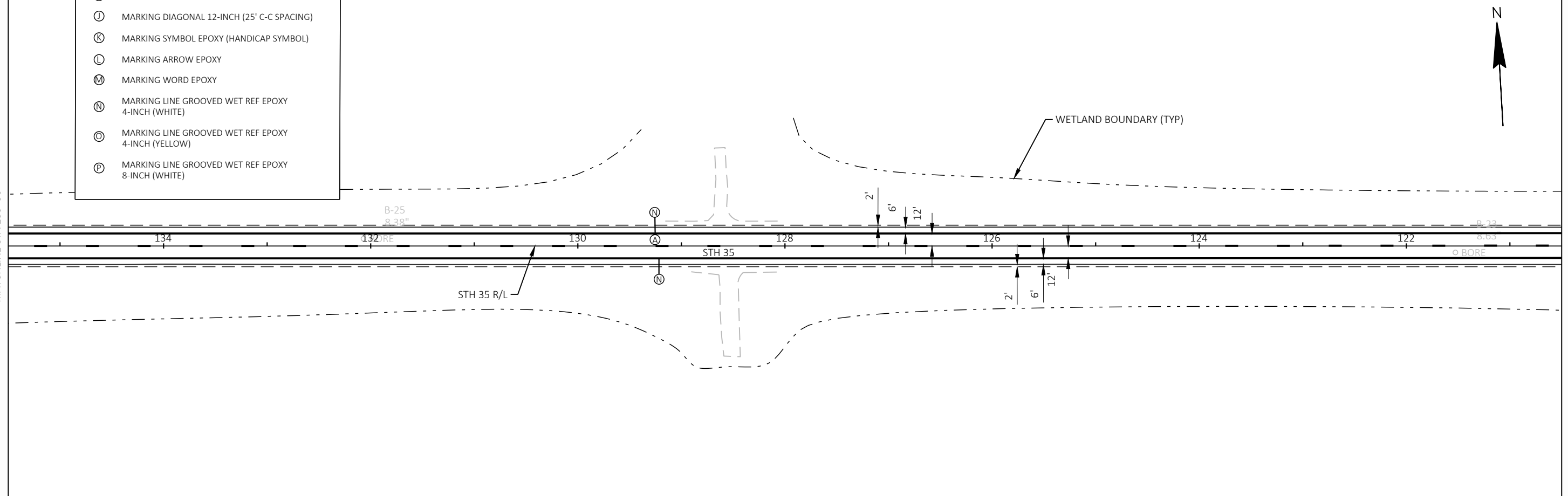
**MARKING LEGEND**

- (A) MARKING LINE EPOXY 4-INCH (SKIP YELLOW)
- (B) MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW)
- (C) MARKING LINE EPOXY 4-INCH (SINGLE YELLOW)
- (D) MARKING PARKING STALL EPOXY (WHITE)
- (E) MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)
- (F) MARKING LINE EPOXY 4-INCH (SINGLE WHITE)
- (G) MARKING LINE EPOXY 4-INCH (SKIP WHITE)
- (H) MARKING CURB EPOXY (YELLOW)
- (I) MARKING STOP LINE EPOXY 18-INCH
- (J) MARKING DIAGONAL 12-INCH (25' C-C SPACING)
- (K) MARKING SYMBOL EPOXY (HANDICAP SYMBOL)
- (L) MARKING ARROW EPOXY
- (M) MARKING WORD EPOXY
- (N) MARKING LINE GROOVED WET REF EPOXY 4-INCH (WHITE)
- (O) MARKING LINE GROOVED WET REF EPOXY 4-INCH (YELLOW)
- (P) MARKING LINE GROOVED WET REF EPOXY 8-INCH (WHITE)

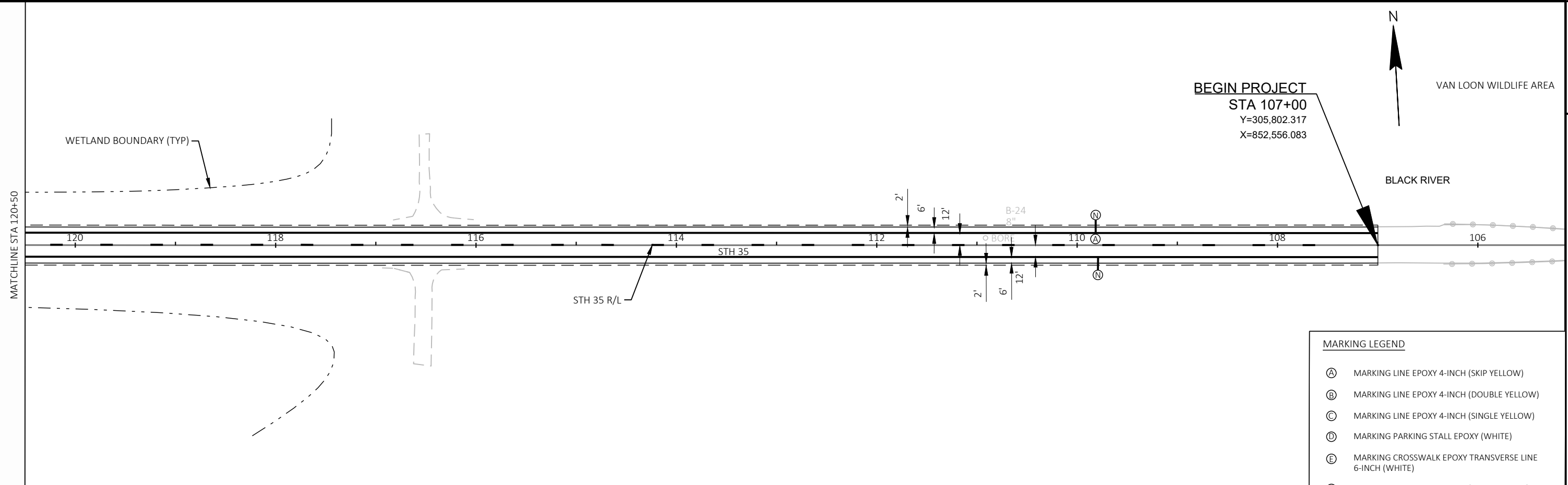


MATCHLINE STA 135+50

MATCHLINE STA 120+50

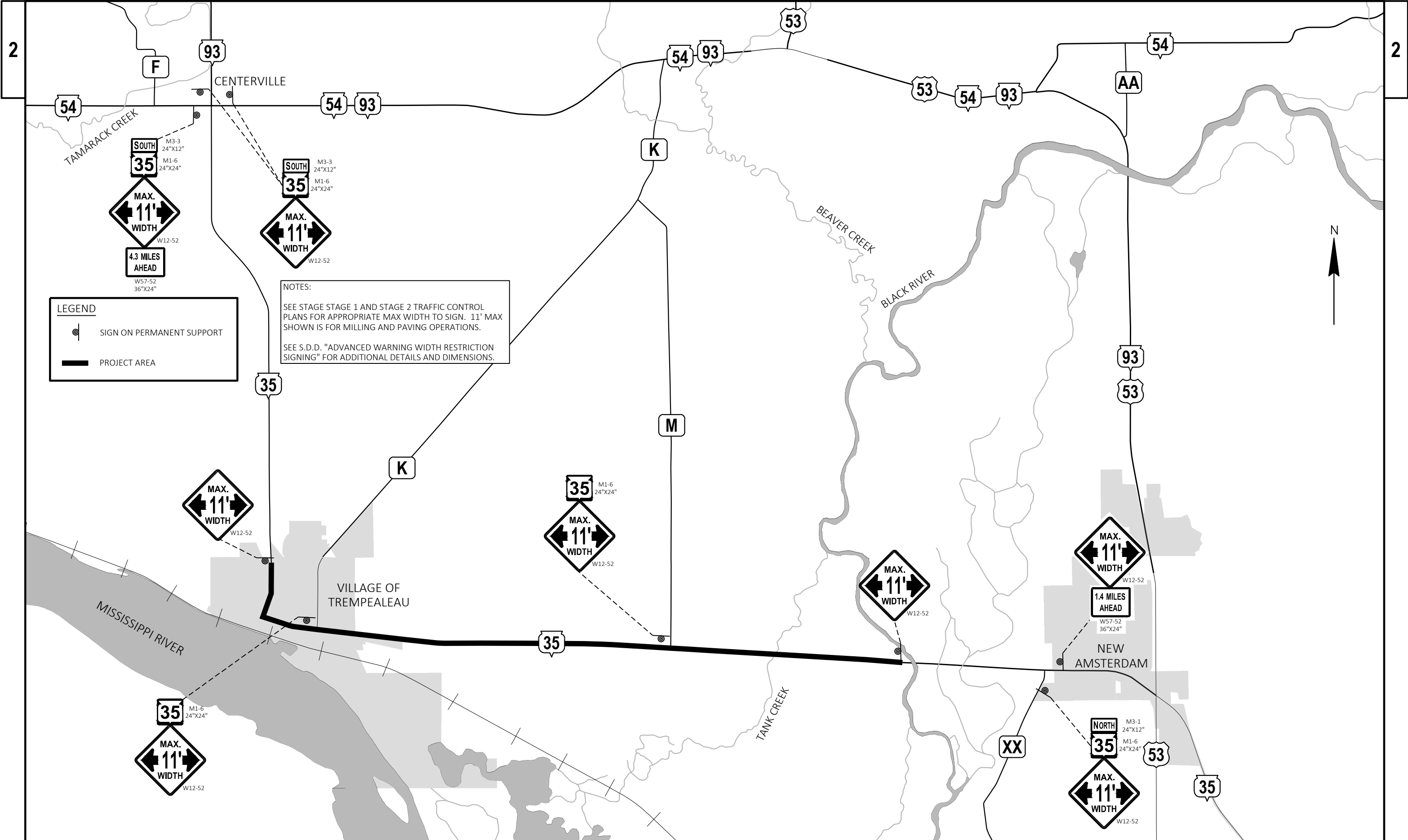






**MARKING LEGEND**

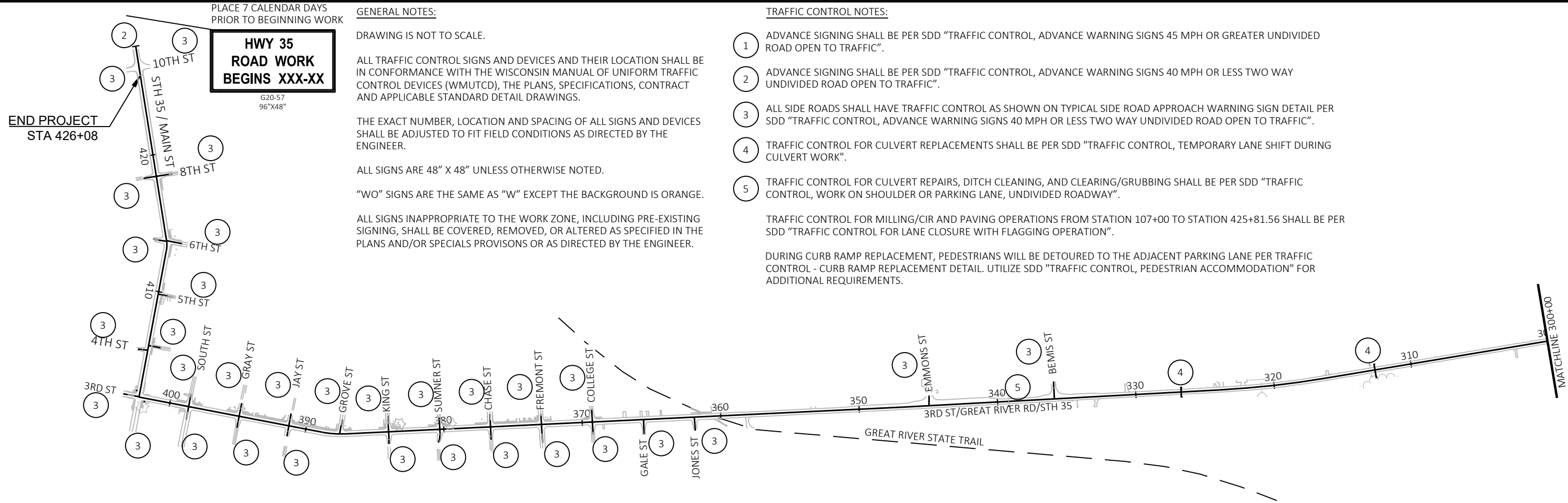
- (A) MARKING LINE EPOXY 4-INCH (SKIP YELLOW)
- (B) MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW)
- (C) MARKING LINE EPOXY 4-INCH (SINGLE YELLOW)
- (D) MARKING PARKING STALL EPOXY (WHITE)
- (E) MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)
- (F) MARKING LINE EPOXY 4-INCH (SINGLE WHITE)
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- (H) MARKING CURB EPOXY (YELLOW)
- (I) MARKING STOP LINE EPOXY 18-INCH
- (J) MARKING DIAGONAL 12-INCH (25' C-C SPACING)
- (K) MARKING SYMBOL EPOXY (HANDICAP SYMBOL)
- (L) MARKING ARROW EPOXY
- (M) MARKING WORD EPOXY
- (N) MARKING LINE GROOVED WET REF EPOXY 4-INCH (WHITE)
- (O) MARKING LINE GROOVED WET REF EPOXY 4-INCH (YELLOW)
- (P) MARKING LINE GROOVED WET REF EPOXY 8-INCH (WHITE)



**LEGEND**

- ⊙ SIGN ON PERMANENT SUPPORT
- PROJECT AREA

**NOTES:**  
 SEE STAGE 1 AND STAGE 2 TRAFFIC CONTROL PLANS FOR APPROPRIATE MAX WIDTH TO SIGN. 11' MAX SHOWN IS FOR MILLING AND PAVING OPERATIONS.  
 SEE S.D.D. "ADVANCED WARNING WIDTH RESTRICTION SIGNING" FOR ADDITIONAL DETAILS AND DIMENSIONS.



PLACE 7 CALENDAR DAYS  
PRIOR TO BEGINNING WORK

**HWY 35  
ROAD WORK  
BEGINS XXX-XX**

G20-57  
96"x48"

**GENERAL NOTES:**

DRAWING IS NOT TO SCALE.

ALL TRAFFIC CONTROL SIGNS AND DEVICES AND THEIR LOCATION SHALL BE IN CONFORMANCE WITH THE WISCONSIN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD), THE PLANS, SPECIFICATIONS, CONTRACT AND APPLICABLE STANDARD DETAIL DRAWINGS.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

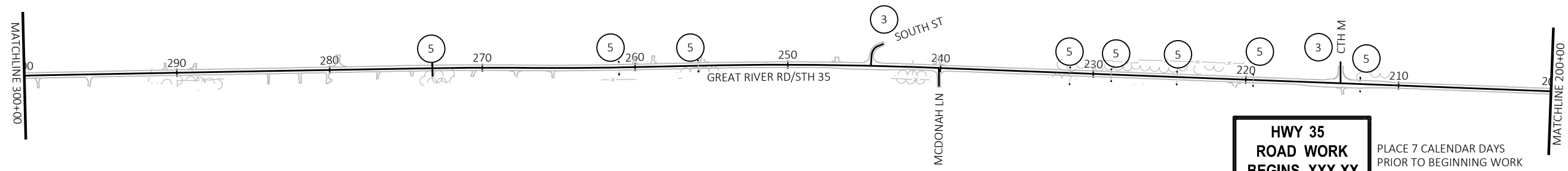
ALL SIGNS INAPPROPRIATE TO THE WORK ZONE, INCLUDING PRE-EXISTING SIGNING, SHALL BE COVERED, REMOVED, OR ALTERED AS SPECIFIED IN THE PLANS AND/OR SPECIALS PROVISIONS OR AS DIRECTED BY THE ENGINEER.

**TRAFFIC CONTROL NOTES:**

- 1 ADVANCE SIGNING SHALL BE PER SDD "TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 MPH OR GREATER UNDIVIDED ROAD OPEN TO TRAFFIC".
- 2 ADVANCE SIGNING SHALL BE PER SDD "TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 MPH OR LESS TWO WAY UNDIVIDED ROAD OPEN TO TRAFFIC".
- 3 ALL SIDE ROADS SHALL HAVE TRAFFIC CONTROL AS SHOWN ON TYPICAL SIDE ROAD APPROACH WARNING SIGN DETAIL PER SDD "TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 MPH OR LESS TWO WAY UNDIVIDED ROAD OPEN TO TRAFFIC".
- 4 TRAFFIC CONTROL FOR CULVERT REPLACEMENTS SHALL BE PER SDD "TRAFFIC CONTROL, TEMPORARY LANE SHIFT DURING CULVERT WORK".
- 5 TRAFFIC CONTROL FOR CULVERT REPAIRS, DITCH CLEANING, AND CLEARING/GRUBBING SHALL BE PER SDD "TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY".

TRAFFIC CONTROL FOR MILLING/CIR AND PAVING OPERATIONS FROM STATION 107+00 TO STATION 425+81.56 SHALL BE PER SDD "TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION".

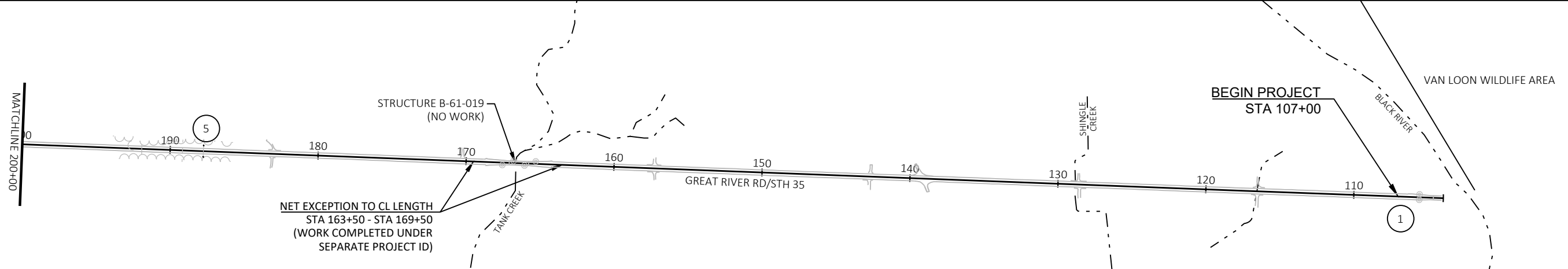
DURING CURB RAMP REPLACEMENT, PEDESTRIANS WILL BE DETOURED TO THE ADJACENT PARKING LANE PER TRAFFIC CONTROL - CURB RAMP REPLACEMENT DETAIL. UTILIZE SDD "TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION" FOR ADDITIONAL REQUIREMENTS.



**HWY 35  
ROAD WORK  
BEGINS XXX-XX**

G20-57  
96"x48"

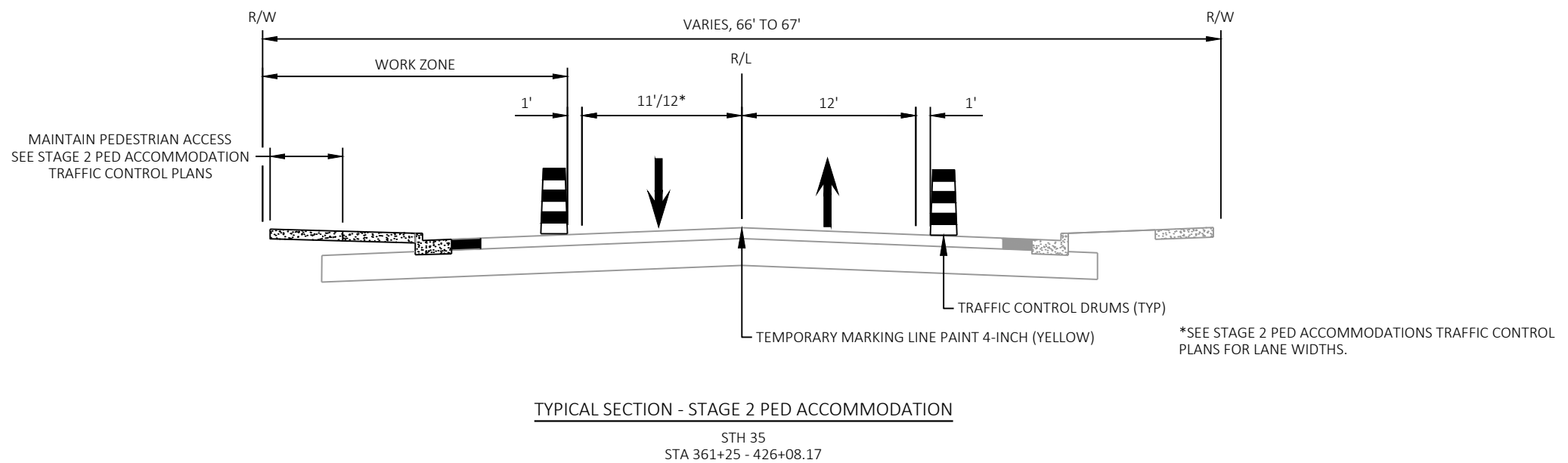
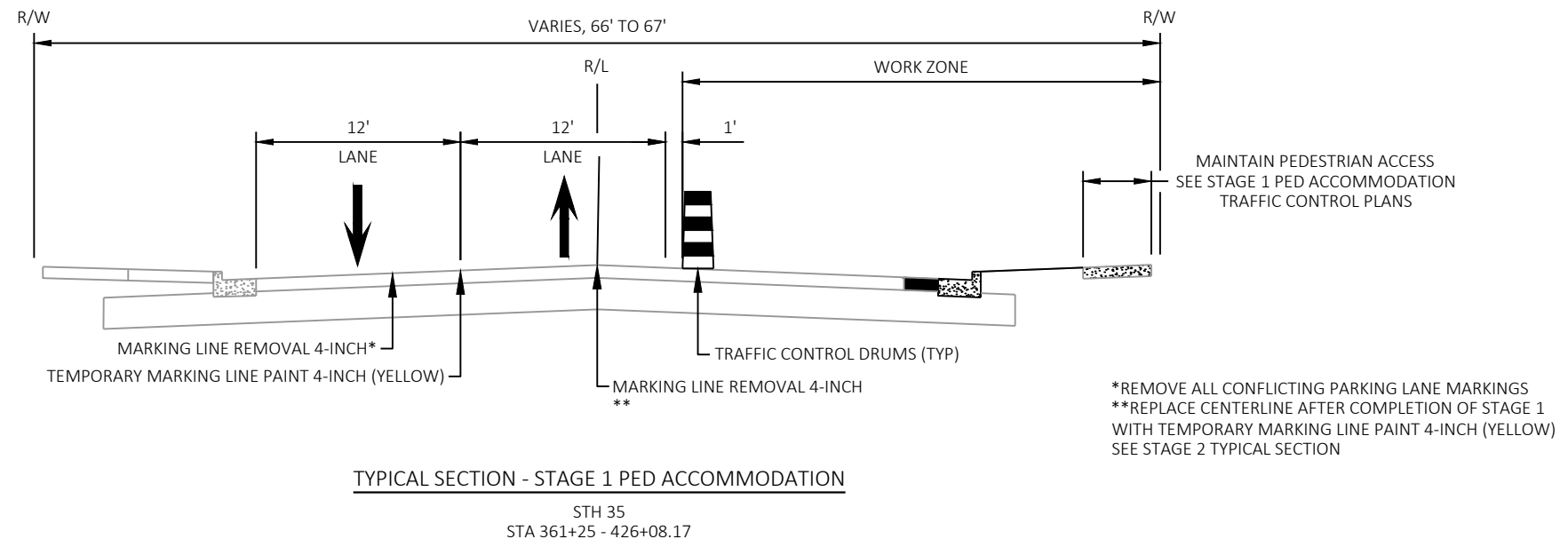
PLACE 7 CALENDAR DAYS  
PRIOR TO BEGINNING WORK



**NET EXCEPTION TO CL LENGTH**  
STA 163+50 - STA 169+50  
(WORK COMPLETED UNDER  
SEPARATE PROJECT ID)

**BEGIN PROJECT  
STA 107+00**

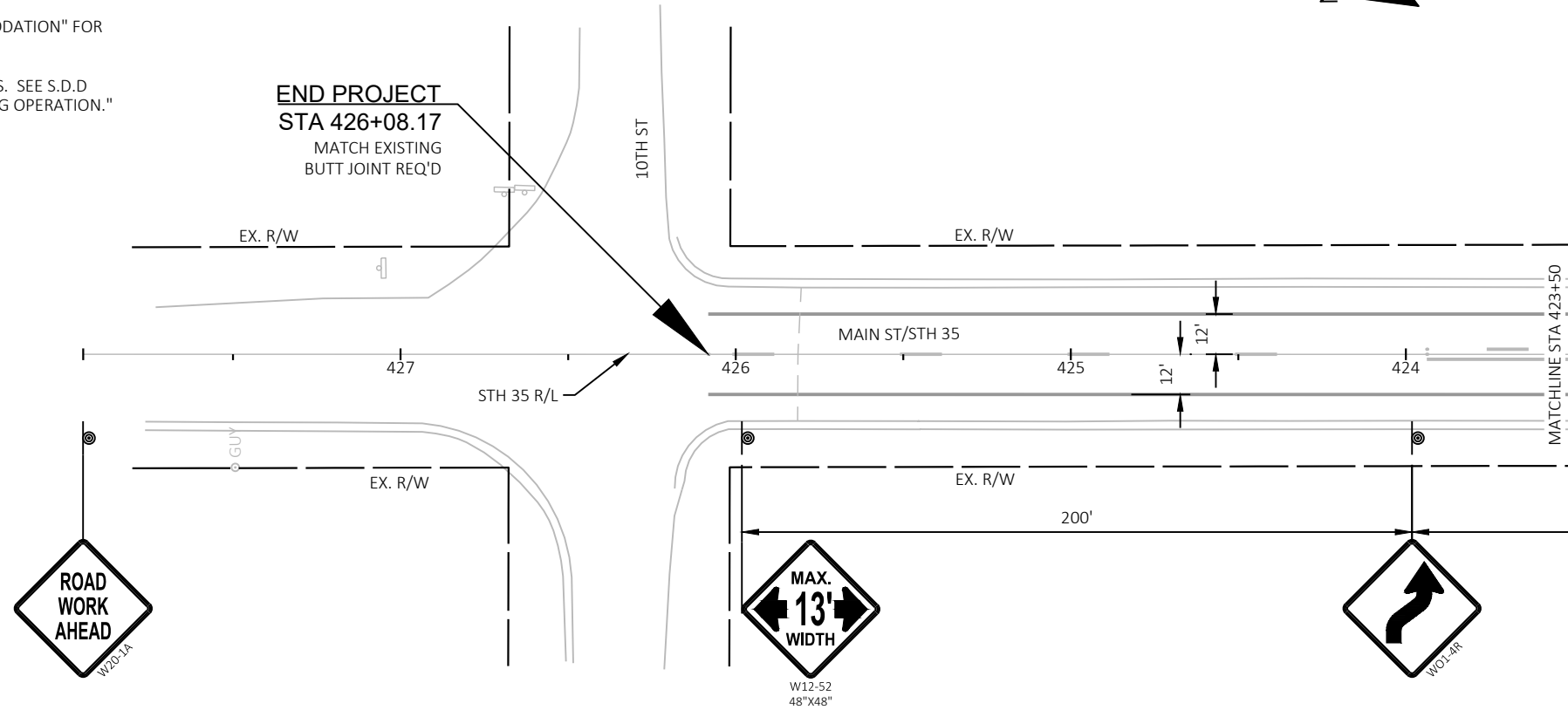




NOTES:

SEE S.D.D. "TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION" FOR ADDITIONAL DETAILS AND DIMENSIONS.

FLAG AS REQUIRED DEPENDING ON WORK OPERATIONS. SEE S.D.D. "TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION."



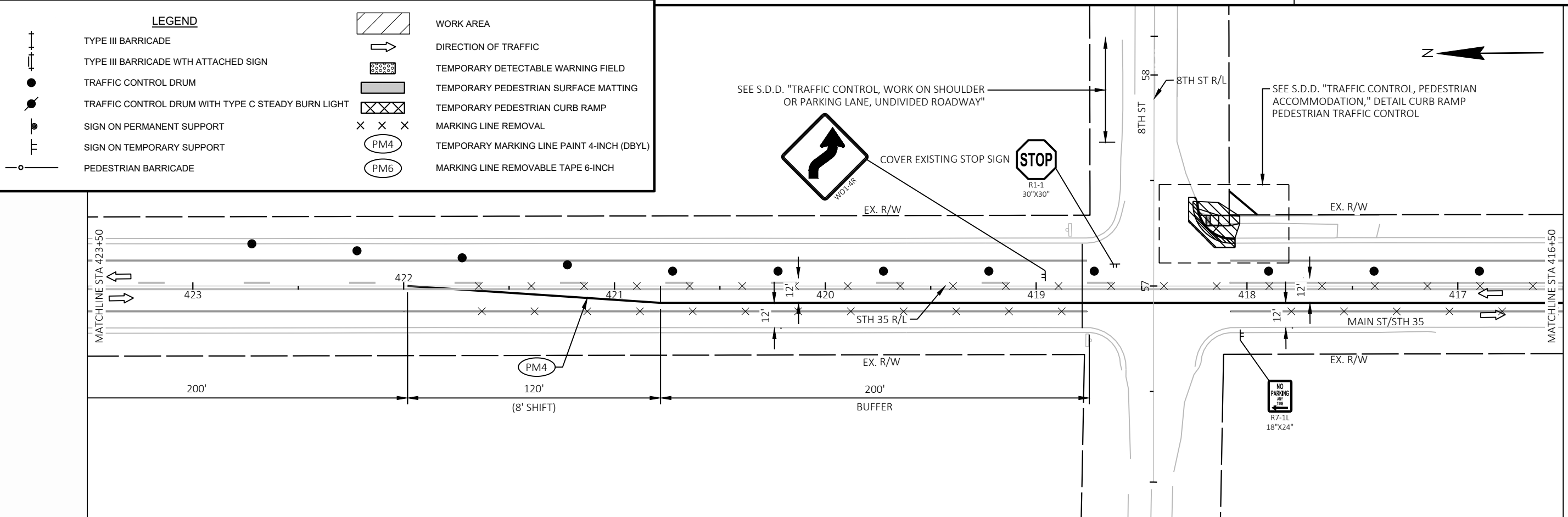
STAGE 1: UTILIZE A SHOULDER/PARKING LANE CLOSURE FOR CURB RAMP CONSTRUCTION AND WATER IMPROVEMENTS. SHIFT TRAFFIC TO THE SOUTH SIDE OF STH 35 DURING CONSTRUCTION OF THE CURB RAMP AND WATER IMPROVEMENTS ON THE NORTH SIDE OF STH 35.

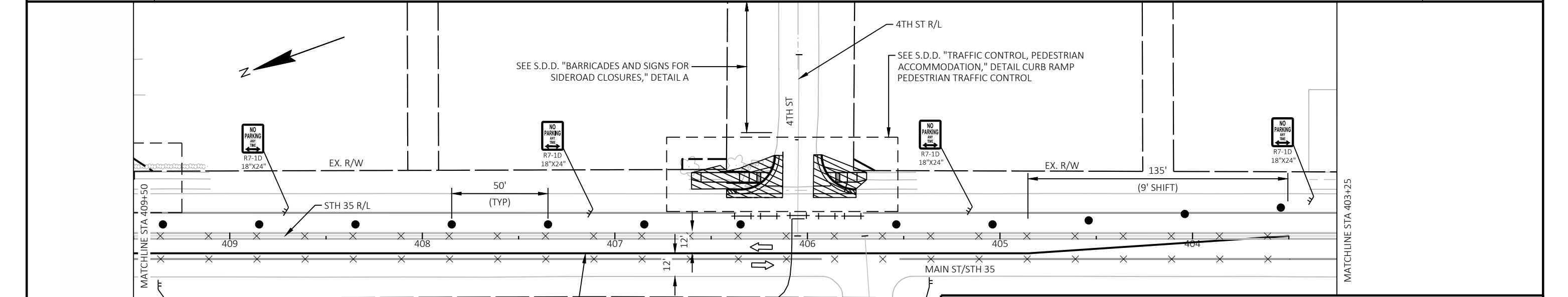
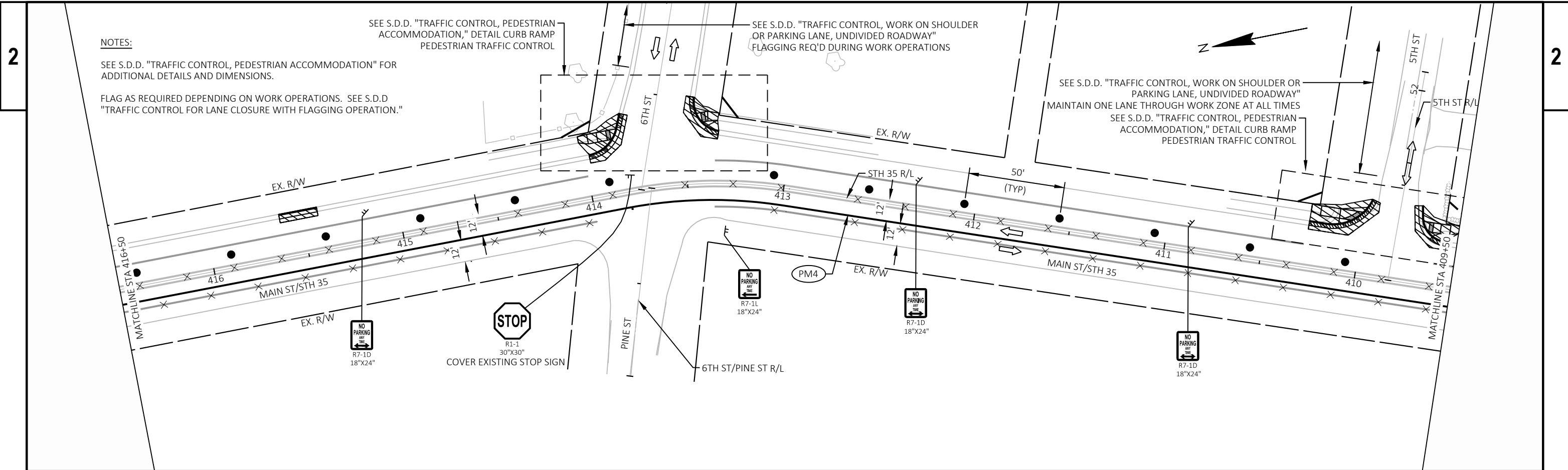
DIRECT PEDESTRIANS AROUND THE CONSTRUCTION WORK ZONE DURING CURB RAMP CONSTRUCTION UTILIZING PEDESTRIAN ACCOMMODATION STANDARD DETAIL DRAWINGS AND AS SHOWN.

DETOUR PEDESTRIANS AWAY FROM THE GREAT RIVER STATE TRAIL CROSSING DURING THE CONSTRUCTION OF THE NORTH TRAIL CURB RAMP AT STH 35 BY CLOSING THE GREAT RIVER STATE TRAIL AT FREMONT STREET. UTILIZE A SIGNED PEDESTRIAN DETOUR FOLLOWING FREMONT STREET AND STH 35 TO THE TRAIL PARKING LOT AND TRAILHEAD ON THE SOUTH SIDE OF THE HIGHWAY. UTILIZE A TEMPORARY PEDESTRIAN CROSSING AROUND THE TRAIL CURB RAMP CONSTRUCTION WORK ZONE WITHIN THE SHOULDER OF WESTBOUND STH 35.

LEGEND

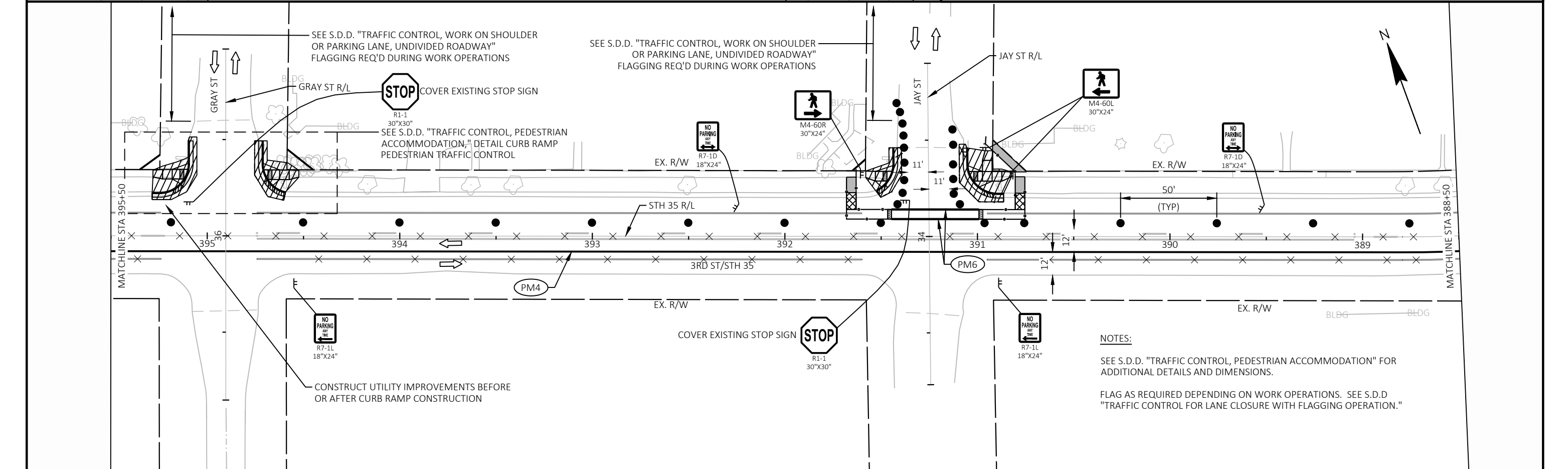
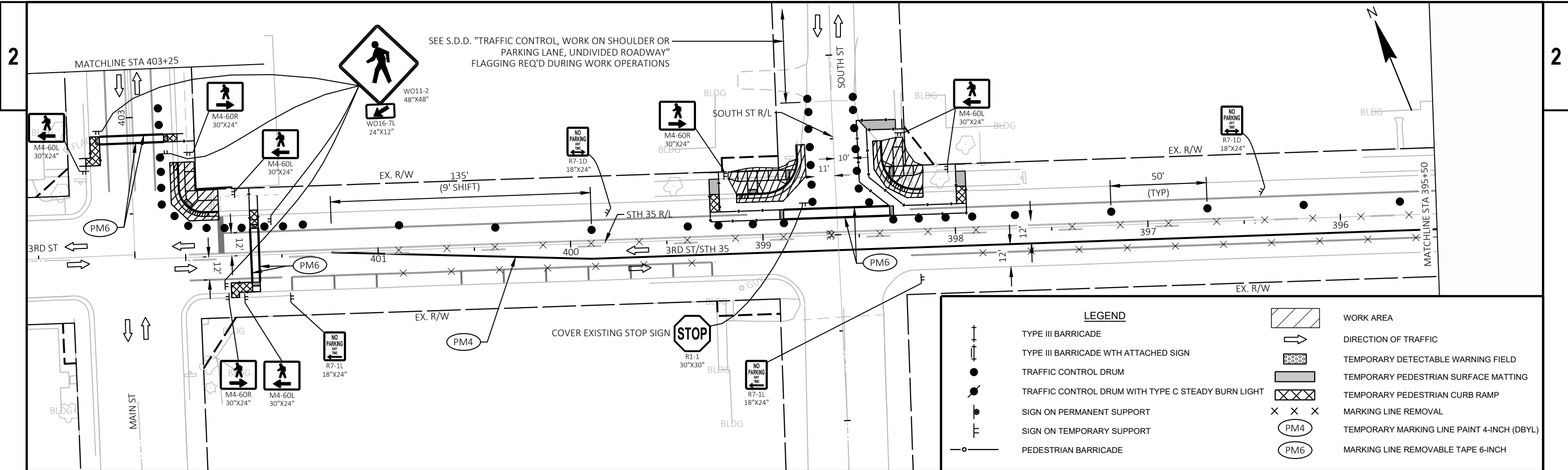
	TYPE III BARRICADE		WORK AREA
	TYPE III BARRICADE WITH ATTACHED SIGN		DIRECTION OF TRAFFIC
	TRAFFIC CONTROL DRUM		TEMPORARY DETECTABLE WARNING FIELD
	TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT		TEMPORARY PEDESTRIAN SURFACE MATTING
	SIGN ON PERMANENT SUPPORT		TEMPORARY PEDESTRIAN CURB RAMP
	SIGN ON TEMPORARY SUPPORT		MARKING LINE REMOVAL
	PEDESTRIAN BARRICADE		TEMPORARY MARKING LINE PAINT 4-INCH (DBYL)
			MARKING LINE REMOVABLE TAPE 6-INCH

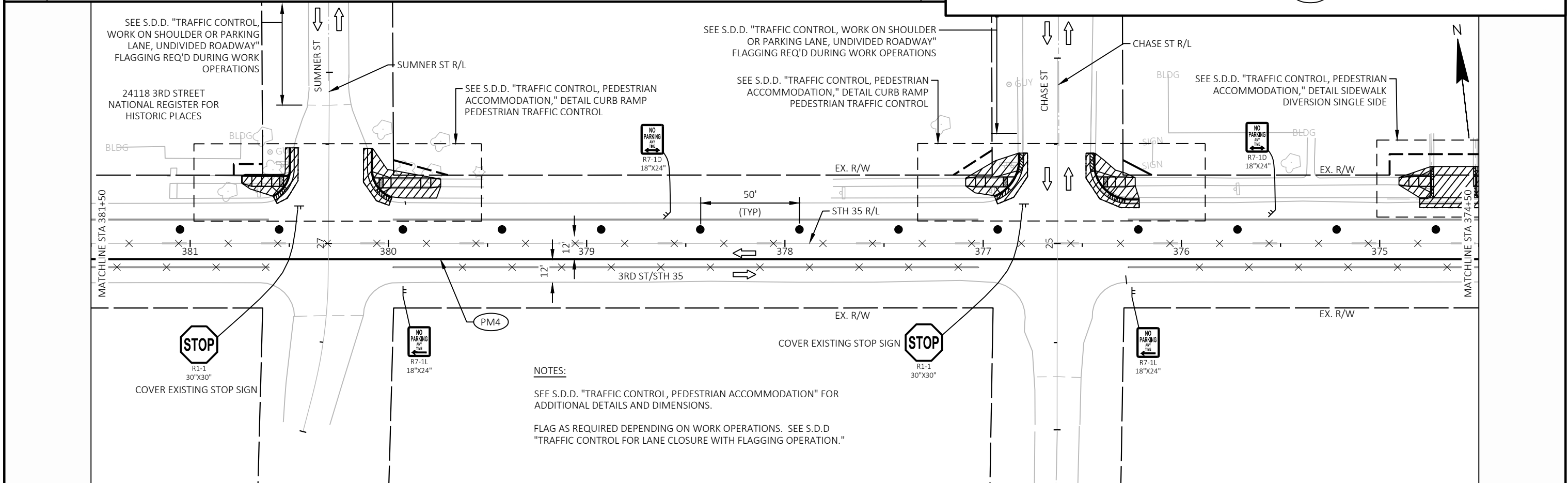
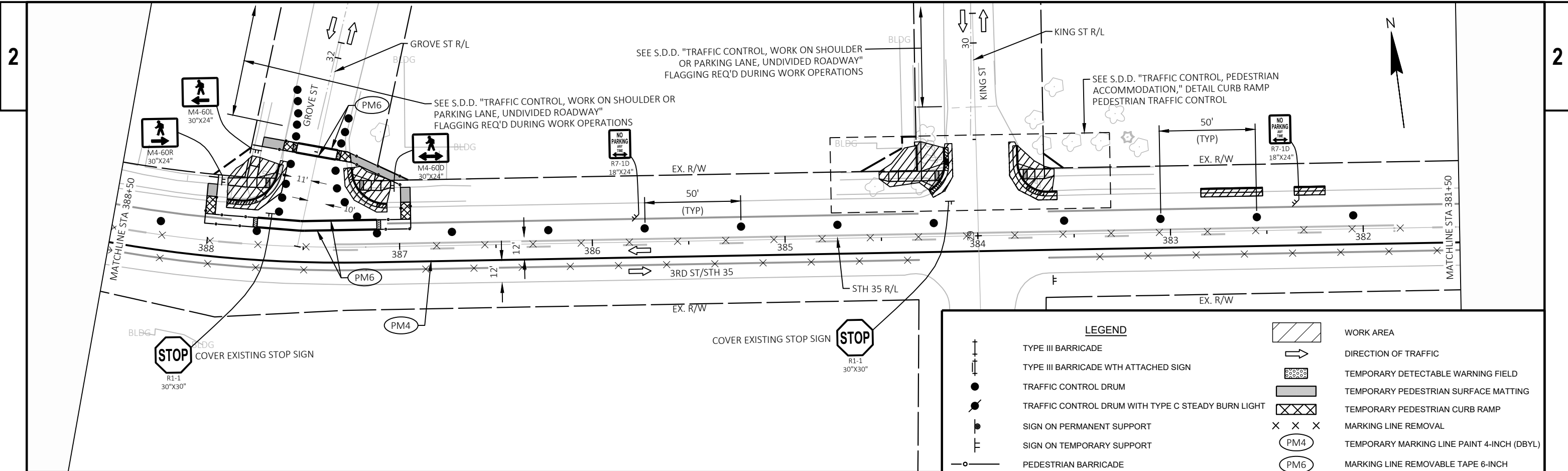




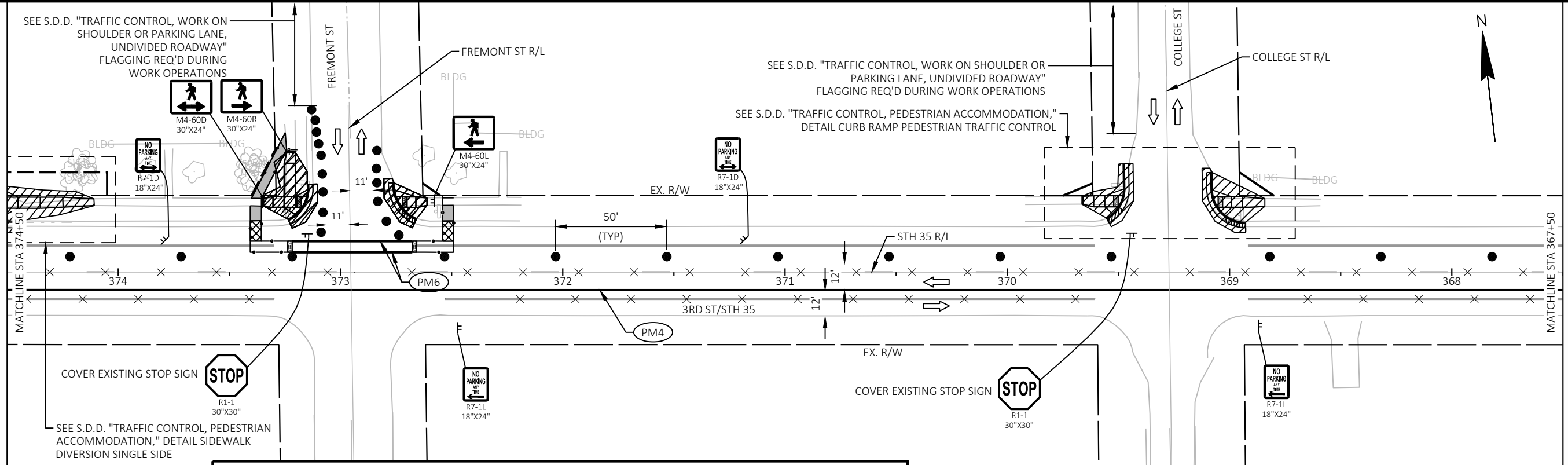
**LEGEND**

	TYPE III BARRICADE		WORK AREA
	TYPE III BARRICADE WITH ATTACHED SIGN		DIRECTION OF TRAFFIC
	TRAFFIC CONTROL DRUM		TEMPORARY DETECTABLE WARNING FIELD
	TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT		TEMPORARY PEDESTRIAN SURFACE MATTING
	SIGN ON PERMANENT SUPPORT		TEMPORARY PEDESTRIAN CURB RAMP
	SIGN ON TEMPORARY SUPPORT		MARKING LINE REMOVAL
	PEDESTRIAN BARRICADE		TEMPORARY MARKING LINE PAINT 4-INCH (DBYL)
			MARKING LINE REMOVABLE TAPE 6-INCH

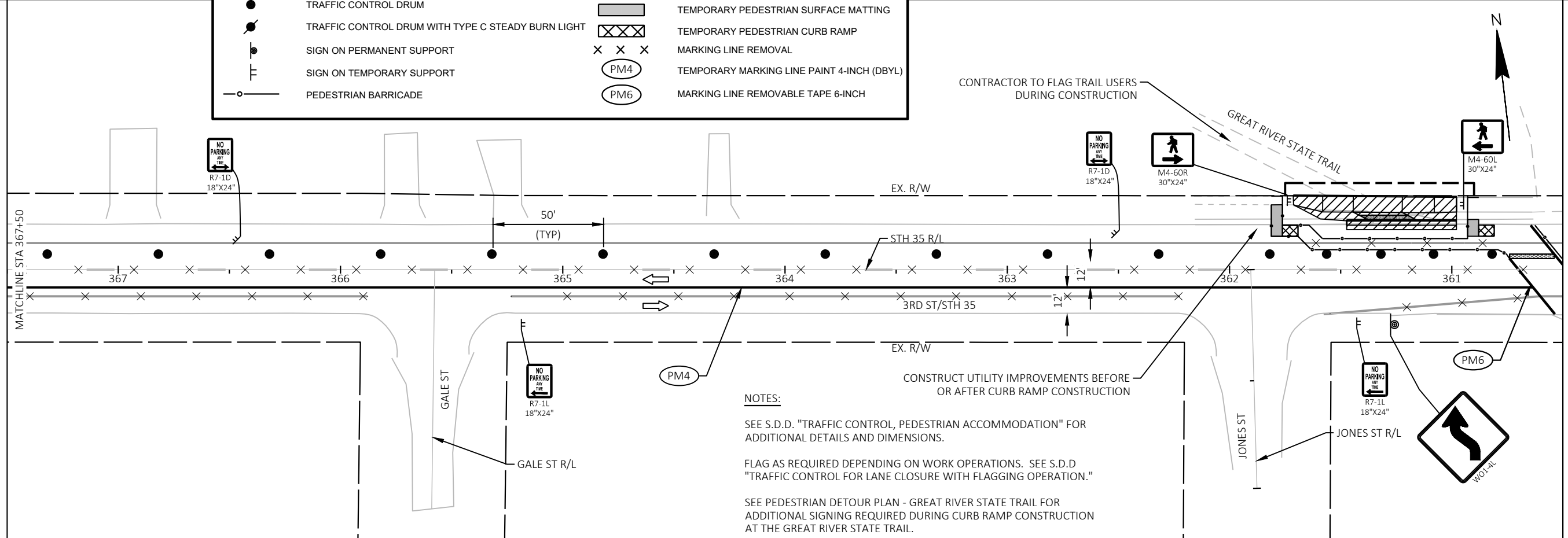








LEGEND	
	TYPE III BARRICADE
	TYPE III BARRICADE WITH ATTACHED SIGN
	TRAFFIC CONTROL DRUM
	TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
	SIGN ON PERMANENT SUPPORT
	SIGN ON TEMPORARY SUPPORT
	PEDESTRIAN BARRICADE
	WORK AREA
	DIRECTION OF TRAFFIC
	TEMPORARY DETECTABLE WARNING FIELD
	TEMPORARY PEDESTRIAN SURFACE MATTING
	TEMPORARY PEDESTRIAN CURB RAMP
	MARKING LINE REMOVAL
	TEMPORARY MARKING LINE PAINT 4-INCH (DBYL)
	MARKING LINE REMOVABLE TAPE 6-INCH

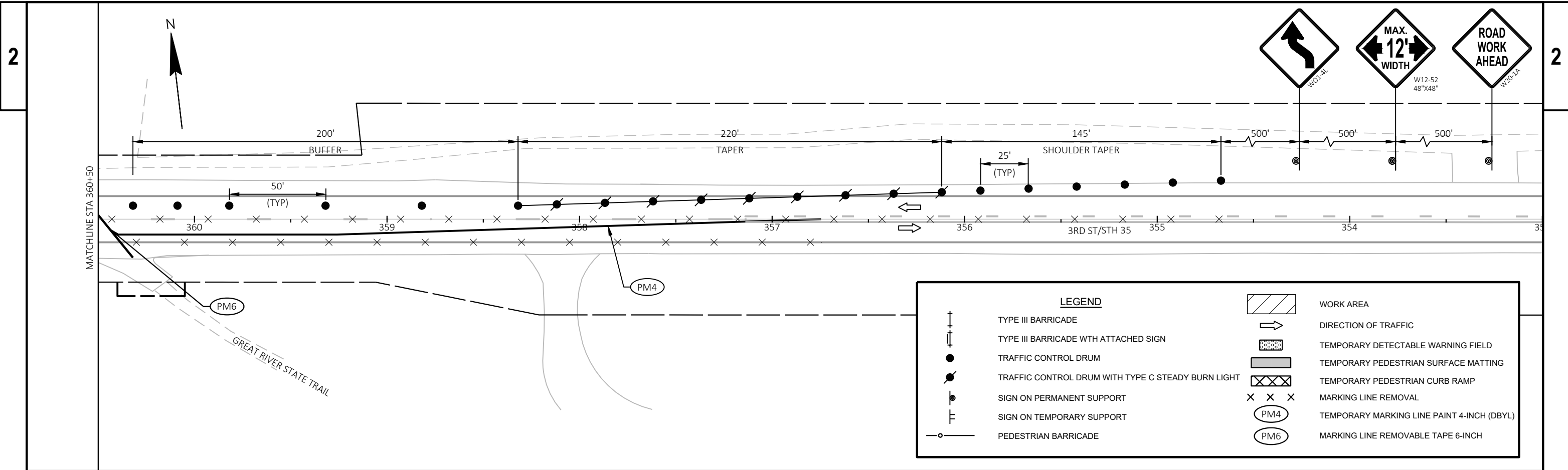


**NOTES:**

SEE S.D.D. "TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION" FOR ADDITIONAL DETAILS AND DIMENSIONS.

FLAG AS REQUIRED DEPENDING ON WORK OPERATIONS. SEE S.D.D. "TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION."

SEE PEDESTRIAN DETOUR PLAN - GREAT RIVER STATE TRAIL FOR ADDITIONAL SIGNING REQUIRED DURING CURB RAMP CONSTRUCTION AT THE GREAT RIVER STATE TRAIL.



**NOTES:**

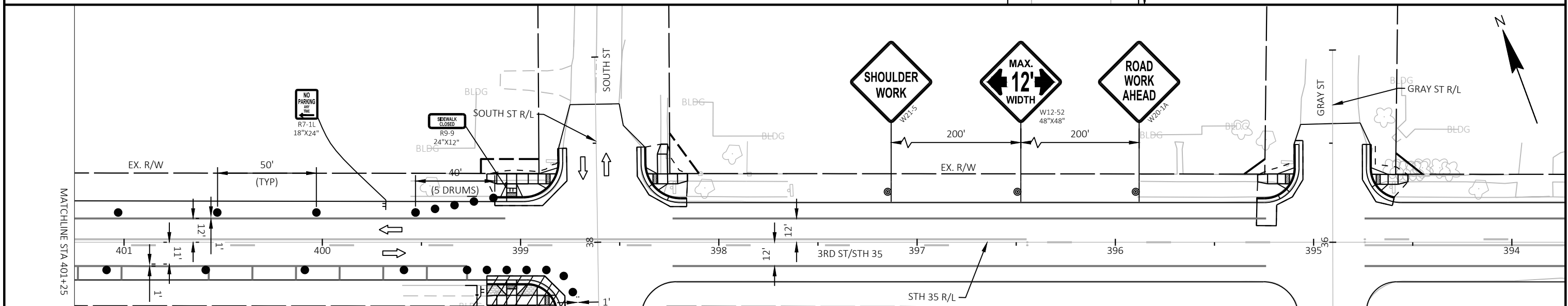
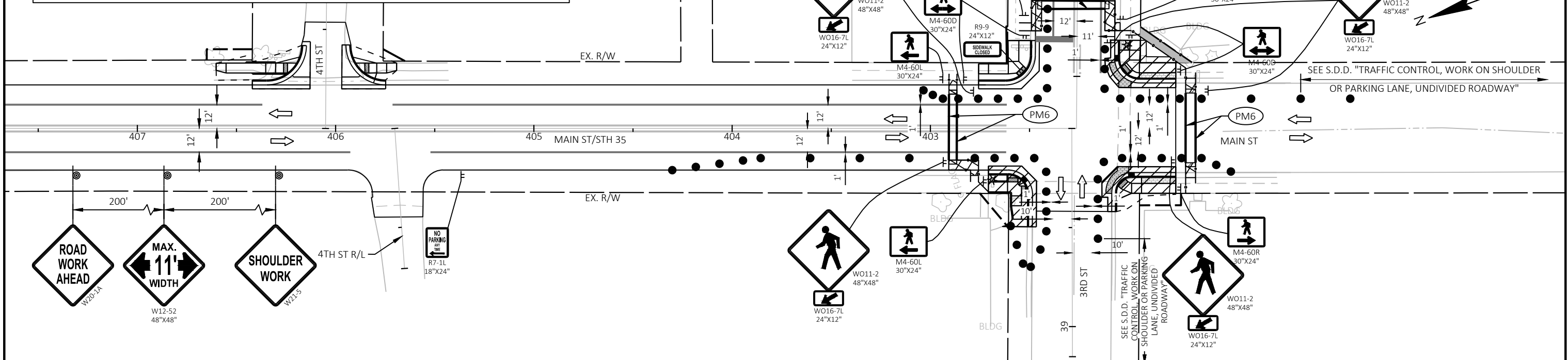
SEE S.D.D. "TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION" FOR ADDITIONAL DETAILS AND DIMENSIONS.

FLAG AS REQUIRED DEPENDING ON WORK OPERATIONS. SEE S.D.D. "TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION."

STAGE 2: UTILIZE A SHOULDER/PARKING LANE CLOSURE FOR CURB RAMP CONSTRUCTION ON THE SOUTH SIDE OF STH 35.

DIRECT PEDESTRIANS AROUND THE CONSTRUCTION WORK ZONE DURING CURB RAMP CONSTRUCTION UTILIZING PEDESTRIAN ACCOMMODATION STANDARD DETAIL DRAWINGS AND AS SHOWN.

DIVERT PEDESTRIANS AWAY FROM THE GREAT RIVER STATE TRAIL CROSSING DURING CONSTRUCTION OF THE SOUTH TRAIL CURB RAMP AT STH 35 UTILIZING THE EXISTING MULTI-USE PATH ALONG THE NORTH SIDE OF STH 35, AND A TEMPORARY PEDESTRIAN CROSSING TO THE TRAIL PARKING LOT AND TRAILHEAD ON THE SOUTH SIDE OF THE HIGHWAY.



NOTES:

SEE S.D.D. "TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION" FOR ADDITIONAL DETAILS AND DIMENSIONS.

FLAG AS REQUIRED DEPENDING ON WORK OPERATIONS. SEE S.D.D. "TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION."

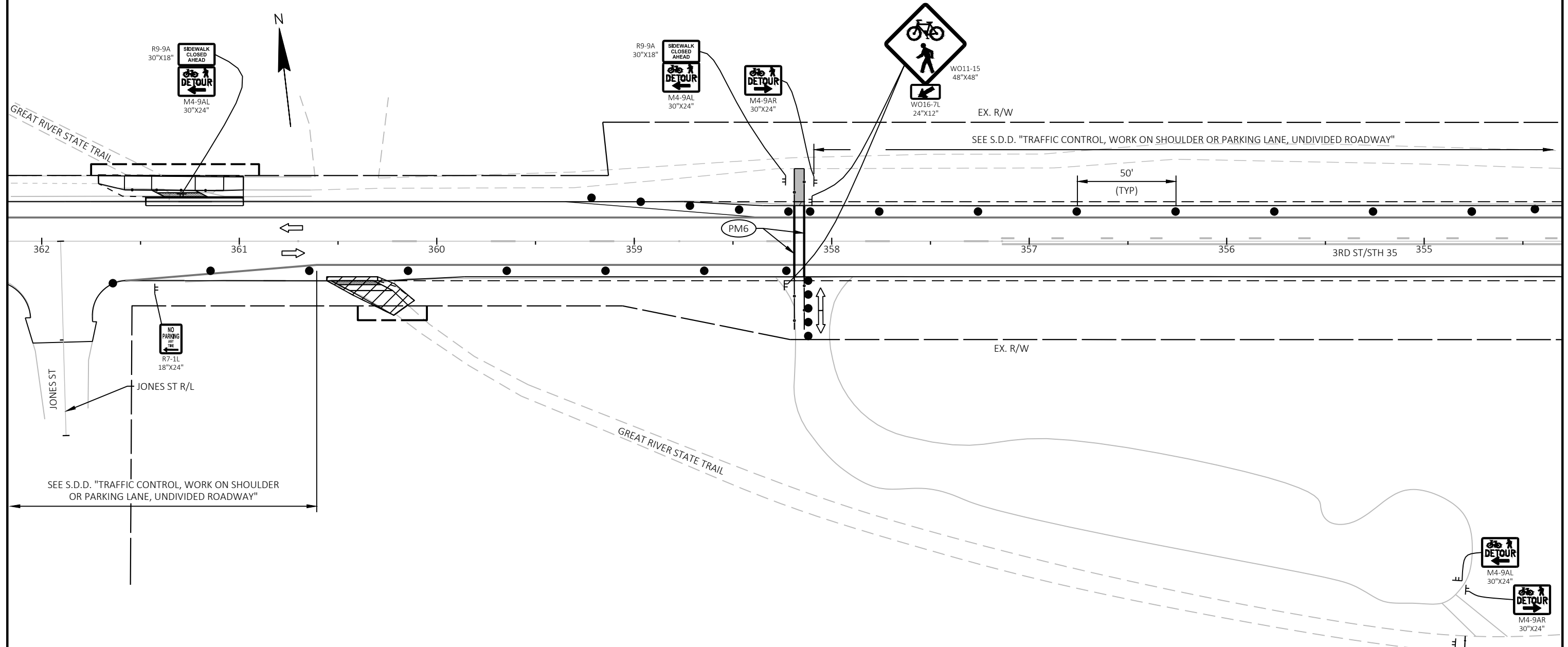
LEGEND

	TYPE III BARRICADE		WORK AREA
	TYPE III BARRICADE WITH ATTACHED SIGN		DIRECTION OF TRAFFIC
	TRAFFIC CONTROL DRUM		TEMPORARY DETECTABLE WARNING FIELD
	TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT		TEMPORARY PEDESTRIAN SURFACE MATTING
	SIGN ON PERMANENT SUPPORT		TEMPORARY PEDESTRIAN CURB RAMP
	SIGN ON TEMPORARY SUPPORT		MARKING LINE REMOVAL
	PEDESTRIAN BARRICADE		TEMPORARY MARKING LINE PAINT 4-INCH (DBYL)
			MARKING LINE REMOVABLE TAPE 6-INCH

NOTES:




SEE S.D.D. "TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION" FOR ADDITIONAL DETAILS AND DIMENSIONS.

FLAG AS REQUIRED DEPENDING ON WORK OPERATIONS. SEE S.D.D. "TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION."



LEGEND	
	TYPE III BARRICADE
	TYPE III BARRICADE WITH ATTACHED SIGN
	TRAFFIC CONTROL DRUM
	TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
	SIGN ON PERMANENT SUPPORT
	SIGN ON TEMPORARY SUPPORT
	PEDESTRIAN BARRICADE
	WORK AREA
	DIRECTION OF TRAFFIC
	TEMPORARY DETECTABLE WARNING FIELD
	TEMPORARY PEDESTRIAN SURFACE MATTING
	TEMPORARY PEDESTRIAN CURB RAMP
	MARKING LINE REMOVAL
	TEMPORARY MARKING LINE PAINT 4-INCH (DBYL)
	MARKING LINE REMOVABLE TAPE 6-INCH

**LEGEND:**

-  GREAT RIVER STATE TRAIL DETOUR ROUTE
-  SIGN ON TEMPORARY SUPPORT
-  WORK ZONE



PROJECT NO: 7140-00-70	HWY: STH 35	COUNTY: TREMPLEALEU	PEDESTRIAN DETOUR PLAN - GREAT RIVER STATE TRAIL	SHEET	<b>E</b>
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Estimate Of Quantities

7140-00-70

Line	Item	Item Description	Unit	Total	Qty
0002	201.0120	Clearing	ID	42.000	42.000
0004	201.0205	Grubbing	STA	2.000	2.000
0006	201.0220	Grubbing	ID	42.000	42.000
0008	203.0100	Removing Small Pipe Culverts	EACH	2.000	2.000
0010	204.0100	Removing Concrete Pavement	SY	14.000	14.000
0012	204.0110	Removing Asphaltic Surface	SY	1,287.000	1,287.000
0014	204.0115	Removing Asphaltic Surface Butt Joints	SY	1,319.000	1,319.000
0016	204.0120	Removing Asphaltic Surface Milling	SY	61,900.000	61,900.000
0018	204.0150	Removing Curb & Gutter	LF	1,198.000	1,198.000
0020	204.0155	Removing Concrete Sidewalk	SY	455.000	455.000
0022	204.0195	Removing Concrete Bases	EACH	2.000	2.000
0024	204.9060.S	Removing (item description) 01. Apron Endwall for CPCS 24-Inch	EACH	1.000	1.000
0026	204.9090.S	Removing (item description) 01. CPRC 24-Inch	LF	8.000	8.000
0028	204.9090.S	Removing (item description) 02. CPRC 72-Inch	LF	8.000	8.000
0030	205.0100	Excavation Common	CY	1,348.000	1,348.000
0032	208.1500.S	Temporary Lane Shift During Culvert Work	EACH	4.000	4.000
0034	211.0101	Prepare Foundation for Asphaltic Paving (project) 01. 7140-00-70	EACH	1.000	1.000
0036	211.0700.S	Prepare Foundation for CIR Base Layer (project) 01. 7140-00-70	EACH	1.000	1.000
0038	211.0800.S	Base Repair for CIR Layer	CY	800.000	800.000
0040	213.0100	Finishing Roadway (project) 01. 7140-00-70	EACH	1.000	1.000
0042	305.0110	Base Aggregate Dense 3/4-Inch	TON	4,970.000	4,970.000
0044	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	1,070.000	1,070.000
0046	305.0500	Shaping Shoulders	STA	494.000	494.000
0048	327.1000.S	CIR Asphaltic Base Layer	SY	94,430.000	94,430.000
0050	405.1000	Stamping Colored Concrete	CY	4.400	4.400
0052	415.0060	Concrete Pavement 6-Inch	SY	14.000	14.000
0054	416.0610	Drilled Tie Bars	EACH	135.000	135.000
0056	455.0605	Tack Coat	GAL	20,824.000	20,824.000
0058	455.0770.S	Asphalt Stabilizing Agent	TON	495.000	495.000
0060	460.0105.S	HMA Percent Within Limits (PWL) Test Strip Volumetrics	EACH	1.000	1.000
0062	460.0110.S	HMA Percent Within Limits (PWL) Test Strip Density	EACH	4.000	4.000
0064	460.6645	HMA Pavement 5 MT 58-34 V	TON	21,725.000	21,725.000
0066	460.9000.S	Material Transfer Vehicle 01. 7140-00-70	EACH	1.000	1.000
0068	465.0105	Asphaltic Surface	TON	994.000	994.000
0070	465.0110	Asphaltic Surface Patching	TON	179.000	179.000
0072	465.0120	Asphaltic Surface Driveways and Field Entrances	TON	131.000	131.000
0074	465.0425	Asphaltic Shoulder Rumble Strips 2-Lane Rural	LF	44,758.000	44,758.000
0076	465.0475	Asphalt Centerline Rumble Strips 2-Lane Rural	LF	21,506.000	21,506.000
0078	520.1024	Apron Endwalls for Culvert Pipe 24-Inch	EACH	2.000	2.000
0080	520.1036	Apron Endwalls for Culvert Pipe 36-Inch	EACH	2.000	2.000
0082	520.2024	Culvert Pipe Temporary 24-Inch	LF	8.000	8.000
0084	520.2030	Culvert Pipe Temporary 30-Inch	LF	8.000	8.000
0086	520.3324	Culvert Pipe Class III-A 24-Inch	LF	88.000	88.000
0088	520.4136	Culvert Pipe Class IV 36-Inch	LF	56.000	56.000
0090	520.8700	Cleaning Culvert Pipes	EACH	5.000	5.000
0092	520.9700.S	Culvert Pipe Liners (size) 01. 36-Inch	LF	86.000	86.000
0094	520.9750.S	Cleaning Culvert Pipes for Liner Verification	EACH	1.000	1.000
0096	521.1024	Apron Endwalls for Culvert Pipe Steel 24-Inch	EACH	1.000	1.000
0098	522.0124	Culvert Pipe Reinforced Concrete Class III 24-Inch	LF	8.000	8.000

Estimate Of Quantities

7140-00-70

Line	Item	Item Description	Unit	Total	Qty
0100	522.0172	Culvert Pipe Reinforced Concrete Class III 72-Inch	LF	8.000	8.000
0102	522.1024	Apron Endwalls for Culvert Pipe Reinforced Concrete 24-Inch	EACH	6.000	6.000
0104	522.1030	Apron Endwalls for Culvert Pipe Reinforced Concrete 30-Inch	EACH	2.000	2.000
0106	522.1054	Apron Endwalls for Culvert Pipe Reinforced Concrete 54-Inch	EACH	2.000	2.000
0108	522.1072	Apron Endwalls for Culvert Pipe Reinforced Concrete 72-Inch	EACH	6.000	6.000
0110	601.0407	Concrete Curb & Gutter 18-Inch Type D	LF	32.000	32.000
0112	601.0409	Concrete Curb & Gutter 30-Inch Type A	LF	31.000	31.000
0114	601.0411	Concrete Curb & Gutter 30-Inch Type D	LF	1,159.000	1,159.000
0116	601.0600	Concrete Curb Pedestrian	LF	121.000	121.000
0118	602.0405	Concrete Sidewalk 4-Inch	SF	4,800.000	4,800.000
0120	602.0505	Curb Ramp Detectable Warning Field Yellow	SF	408.000	408.000
0122	602.0605	Curb Ramp Detectable Warning Field Radial Yellow	SF	17.000	17.000
0124	606.0200	Riprap Medium	CY	25.000	25.000
0126	606.0400	Riprap Extra-Heavy	CY	11.000	11.000
0128	611.0530	Manhole Covers Type J	EACH	6.000	6.000
0130	611.8110	Adjusting Manhole Covers	EACH	10.000	10.000
0132	611.8115	Adjusting Inlet Covers	EACH	20.000	20.000
0134	611.8120.S	Cover Plates Temporary	EACH	33.000	33.000
0136	611.9800.S	Pipe Grates	EACH	1.000	1.000
0138	618.0100	Maintenance And Repair of Haul Roads (project) 01. 7140-00-70	EACH	1.000	1.000
0140	619.1000	Mobilization	EACH	1.000	1.000
0142	624.0100	Water	MGAL	89.000	89.000
0144	625.0100	Topsoil	SY	6,450.000	6,450.000
0146	628.1504	Silt Fence	LF	8,275.000	8,275.000
0148	628.1520	Silt Fence Maintenance	LF	8,275.000	8,275.000
0150	628.1905	Mobilizations Erosion Control	EACH	4.000	4.000
0152	628.1910	Mobilizations Emergency Erosion Control	EACH	2.000	2.000
0154	628.2004	Erosion Mat Class I Type B	SY	5,850.000	5,850.000
0156	628.2006	Erosion Mat Urban Class I Type A	SY	600.000	600.000
0158	628.7005	Inlet Protection Type A	EACH	5.000	5.000
0160	628.7015	Inlet Protection Type C	EACH	45.000	45.000
0162	628.7504	Temporary Ditch Checks	LF	200.000	200.000
0164	628.7555	Culvert Pipe Checks	EACH	17.000	17.000
0166	628.7570	Rock Bags	EACH	175.000	175.000
0168	629.0210	Fertilizer Type B	CWT	4.500	4.500
0170	630.0110	Seeding Mixture No. 10	LB	90.000	90.000
0172	630.0140	Seeding Mixture No. 40	LB	10.000	10.000
0174	630.0200	Seeding Temporary	LB	25.000	25.000
0176	630.0500	Seed Water	MGAL	115.000	115.000
0178	633.5200	Markers Culvert End	EACH	24.000	24.000
0180	634.0618	Posts Wood 4x6-Inch X 18-FT	EACH	2.000	2.000
0182	638.2102	Moving Signs Type II	EACH	11.000	11.000
0184	638.3000	Removing Small Sign Supports	EACH	1.000	1.000
0186	638.4000	Moving Small Sign Supports	EACH	8.000	8.000
0188	642.5001	Field Office Type B	EACH	1.000	1.000
0190	643.0300	Traffic Control Drums	DAY	15,100.000	15,100.000
0192	643.0420	Traffic Control Barricades Type III	DAY	200.000	200.000
0194	643.0705	Traffic Control Warning Lights Type A	DAY	400.000	400.000
0196	643.0715	Traffic Control Warning Lights Type C	DAY	300.000	300.000

Estimate Of Quantities

7140-00-70

Line	Item	Item Description	Unit	Total	Qty
0198	643.0900	Traffic Control Signs	DAY	8,800.000	8,800.000
0200	643.0920	Traffic Control Covering Signs Type II	EACH	12.000	12.000
0202	643.1000	Traffic Control Signs Fixed Message	SF	64.000	64.000
0204	643.3105	Temporary Marking Line Paint 4-Inch	LF	25,038.000	25,038.000
0206	643.3120	Temporary Marking Line Epoxy 4-Inch	LF	22,551.000	22,551.000
0208	643.5000	Traffic Control	EACH	1.000	1.000
0210	644.1440	Temporary Pedestrian Surface Matting	SF	1,025.000	1,025.000
0212	644.1601	Temporary Pedestrian Curb Ramp	DAY	900.000	900.000
0214	644.1605	Temporary Pedestrian Detectable Warning Field	SF	410.000	410.000
0216	644.1810	Temporary Pedestrian Barricade	LF	5,500.000	5,500.000
0218	645.0120	Geotextile Type HR	SY	100.000	100.000
0220	646.1020	Marking Line Epoxy 4-Inch	LF	24,796.000	24,796.000
0222	646.1040	Marking Line Grooved Wet Ref Epoxy 4-Inch	LF	48,617.000	48,617.000
0224	646.3040	Marking Line Grooved Wet Ref Epoxy 8-Inch	LF	1,152.000	1,152.000
0226	646.4520	Marking Line Same Day Epoxy 4-Inch	LF	7,035.000	7,035.000
0228	646.5020	Marking Arrow Epoxy	EACH	2.000	2.000
0230	646.5120	Marking Word Epoxy	EACH	1.000	1.000
0232	646.5220	Marking Symbol Epoxy	EACH	1.000	1.000
0234	646.6120	Marking Stop Line Epoxy 18-Inch	LF	20.000	20.000
0236	646.7120	Marking Diagonal Epoxy 12-Inch	LF	325.000	325.000
0238	646.7420	Marking Crosswalk Epoxy Transverse Line 6-Inch	LF	417.000	417.000
0240	646.8120	Marking Curb Epoxy	LF	2,000.000	2,000.000
0242	646.8320	Marking Parking Stall Epoxy	LF	306.000	306.000
0244	646.9000	Marking Removal Line 4-Inch	LF	12,100.000	12,100.000
0246	650.6000	Construction Staking Pipe Culverts	EACH	2.000	2.000
0248	650.8000	Construction Staking Resurfacing Reference	LF	31,908.000	31,908.000
0250	650.8501	Construction Staking Electrical Installations (project) 01. 7140-00-70	EACH	1.000	1.000
0252	650.9000	Construction Staking Curb Ramps	EACH	37.000	37.000
0254	650.9500	Construction Staking Sidewalk (project) 01. 7140-00-70	EACH	1.000	1.000
0256	650.9911	Construction Staking Supplemental Control (project) 01. 7140-00-70	EACH	1.000	1.000
0258	652.0215	Conduit Rigid Nonmetallic Schedule 40 1 1/4-Inch	LF	30.000	30.000
0260	654.0105	Concrete Bases Type 5	EACH	2.000	2.000
0262	655.0610	Electrical Wire Lighting 12 AWG	LF	250.000	250.000
0264	655.0625	Electrical Wire Lighting 6 AWG	LF	500.000	500.000
0266	690.0150	Sawing Asphalt	LF	2,553.000	2,553.000
0268	690.0250	Sawing Concrete	LF	344.000	344.000
0270	715.0720	Incentive Compressive Strength Concrete Pavement	DOL	500.000	500.000
0272	740.0440	Incentive IRI Ride	DOL	23,740.000	23,740.000
0274	SPV.0055	Special 01. Incentive Density PWL HMA Pavement	DOL	13,400.000	13,400.000
0276	SPV.0055	Special 02. Incentive Air Voids HMA Pavement	DOL	16,800.000	16,800.000
0278	SPV.0055	Special 03. Incentive Density HMA Pavement Longitudinal Joints	DOL	33,170.000	33,170.000
0280	SPV.0060	Special 01. 6-Inch Gate Valve and Box	EACH	9.000	9.000
0282	SPV.0060	Special 02. Hydrant	EACH	2.000	2.000
0284	SPV.0060	Special 03. Relocate Existing Light Pole	EACH	2.000	2.000
0286	SPV.0060	Special 04. Adjusting Water Valve Box	EACH	11.000	11.000
0288	SPV.0060	Special 05. Adjusting Sanitary Manhole Cover	EACH	17.000	17.000
0290	SPV.0060	Special 06. Grading and Shaping Apron Endwall Installation	EACH	17.000	17.000
0292	SPV.0060	Special 07. Grading and Shaping Curb Ramps	EACH	37.000	37.000
0294	SPV.0060	Special 08. Inlet Covers, Type H-D	EACH	4.000	4.000



Estimate Of Quantities

7140-00-70

Line	Item	Item Description	Unit	Total	Qty
0296	SPV.0090	Special 01. Ditch Cleaning	LF	1,075.000	1,075.000
0298	SPV.0090	Special 02. Concrete Gutter 48-inch	LF	27.000	27.000
0300	SPV.0090	Special 03. Temporary Marking Line Removable Tape 6-Inch	LF	1,000.000	1,000.000

3

**201 - CLEARING GRUBBING**

CATEGORY	STATION	TO	STATION	LOCATION	201.0120 CLEARING ID	201.0205 GRUBBING STA	201.0220 GRUBBING ID
0010	260+00	-	262+00	STH 35 LT	-	2	-
0010	402+60	-	402+65	STH 35 LT	42	-	42
TOTAL 0010					42	2	42

**205.0100 COMMON**

CATEGORY	STATION	TO	STATION	LOCATION	205.0100 EXCAVATION COMMON CY	REMARKS
0010	312+06	-	313+21	STH 35	675	CULVERT 180
0010	326+14	-	327+31	STH 35	673	CULVERT 210
TOTAL 0010					1,348	

\*1.25 EXPANSION FACTOR

3

**204.0115 BUTT JOINTS**

CATEGORY	LOCATION	204.0115 REMOVING ASPHALTIC SURFACE BUTT JOINTS SY
0010	107+00	40
0010	139+51, LT	5
0010	163+50	40
0010	169+50	40
0010	CTH M	36
0010	MCDONAH LN	23
0010	SOUTH ST	28
0010	BEMIS ST	33
0010	EMMONS ST	45
0010	JONES	37
0010	GALE ST	25
0010	COLLEGE ST	55
0010	FREMONT ST	77
0010	CHASE ST	65
0010	SUMNER ST	67
0010	KING ST	65
0010	GROVE ST	36
0010	JAY ST	92
0010	GRAY ST	78
0010	SOUTH ST	87
0010	3RD ST	40
0010	MAIN ST	46
0010	4TH ST	53
0010	5TH ST	25
0010	6TH ST	72
0010	8TH ST	64
0010	425+81	45
TOTAL 0010		1,319

**208.1500.S - LANE SHIFT**

CATEGORY	STATION	TO	STATION	LOCATION	208.1500.S TEMPORARY LANE SHIFT DURING CULVERT WORK EACH	REMARKS
0010	312+06	-	313+21	STH 35	2	CULVERT 180
0010	326+14	-	327+31	STH 35	2	CULVERT 210
TOTAL 0010					4	

**211.0800.S BASE REPAIR**

CATEGORY	STATION	TO	STATION	LOCATION	211.0800.S BASE REPAIR FOR CIR LAYER CY	REMARKS
0010	107+00	-	329+75	STH 35	800	UNDISTRIBUTED
TOTAL 0010					800	

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**305 - 465 CULVERT PATCHES**

CATEGORY	STATION	TO	STATION	LOCATION	305.0110 BASE AGGREGATE DENSE 3/4-INCH TON	305.0120 BASE AGGREGATE DENSE 1 1/4- INCH TON	455.0605 TACK COAT GAL	465.0105 ASPHALTIC SURFACE TON	690.0150 SAWING ASPHALT LF	REMARKS
0010	312+06	-	313+21	35	30	390	46	155	72	CULVERT 180
0010	326+14	-	327+31	35	30	395	48	160	72	CULVERT 210
TOTAL 0010					60	785	94	315	144	

\* ADDITIONAL QUANTITIES LISTED ELSEWHERE

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**305.0500 - SHAPING SHOULDERS**

CATEGORY	STATION	TO	STATION	LOCATION	305.0500 SHAPING SHOULDERS STA	REMARKS
0010	107+00	-	163+50	STH 35	114	LT & RT
0010	169+50	-	358+75	STH 35	380	LT & RT
TOTAL 0010					494	

**460 HMA**

CATEGORY	STATION	TO	STATION	LOCATION	204.0120 REMOVING ASPHALTIC SURFACE MILLING SY	327.1000.S CIR ASPHALTIC BASE LAYER SY	455.0605 TACK COAT GAL	455.0770.S ASPHALT STABILIZING AGENT TON	460.6645 HMA PAVEMENT 5 MT 58-34 V TON	REMARKS
0010	107+00	-	163+50	STH 35	23,000	22,600	3,885	115	3,550	
0010	169+50	-	209+36	STH 35	-	15,200	2,660	80	2,475	
0010	209+36	-	217+69	STH 35/CTH M	1,800	280	455	5	675	
0010	217+69	-	238+50	STH 35	-	8,330	1,420	45	1,300	
0010	238+50	-	248+30	STH 35/SOUTH ST	890	3,550	605	20	550	
0010	248+30	-	331+25	STH 35	-	33,200	5,645	170	5,125	
0010	331+25	-	352+30	STH 35/BEMIS/EMMONS	2,810	8,680	1,480	45	1,350	
0010	352+30	-	358+75	STH 35	-	2,590	445	15	400	
0010	358+75	-	426+08	STH 35 (URBAN)	25,900	-	3,235	-	4,925	
TOTAL 0010					54,400	94,430	19,830	495	20,350	
0020	358+75	-	426+08	35 PARKING LANES	7,500	-	900	-	1,375	
TOTAL 0020					7,500	0	900	0	1,375	
PROJECT TOTAL					61,900	94,430	20,730	495	21,725	

\* ADDITIONAL QUANTITIES LISTED ELSEWHERE

**305.0110 BAD SHOULDER**

CATEGORY	STATION	TO	STATION	LOCATION	305.0110 BASE AGGREGATE DENSE 3/4-INCH TON	REMARKS
0010	169+50	-	209+36	STH 35	1,250	LT & RT
0010	209+36	-	217+69	STH 35	20	LT & RT
0010	217+69	-	238+50	STH 35	650	LT & RT
0010	238+50	-	248+30	STH 35	20	LT & RT
0010	248+30	-	331+25	STH 35	2,590	LT & RT
0010	331+25	-	352+30	STH 35	30	LT & RT
0010	352+30	-	358+75	STH 35	210	LT & RT
TOTAL 0010					4,770	

PWL TABLE								
Location	Station	Mixture Use	Underlying Surface	Bid Item	Tons	Thickness	Quality Management Program to be used for:	
							Mixture Acceptance	Density Acceptance
12-Ft Driving Lanes - STH 35	107+00 - 163+50	Lower Layer	CIR	5 MT 58-34 V	1,055	1.25"	PWL Incentive Air Voids Item SPV.0055.02	Incentive Density PWL HMA Pavement Item SPV.0055.01
12-Ft Driving Lanes - STH 35	107+00 - 163+50	Upper Layer	5 MT 58-34 V	5 MT 58-34 V	1,270	1.5"	PWL Incentive Air Voids Item SPV.0055.02	Incentive Density PWL HMA Pavement Item SPV.0055.01
Shoulders - STH 35	107+00 - 163+50	Lower Layer	CIR	5 MT 58-34 V	560	1.25"	PWL Incentive Air Voids Item SPV.0055.02	Acceptance testing by the Department; Not eligible for incentive
Shoulders - STH 35	107+00 - 163+50	Upper Layer	5 MT 58-34 V	5 MT 58-34 V	665	1.5"	PWL Incentive Air Voids Item SPV.0055.02	Acceptance testing by the Department; Not eligible for incentive
12-Ft Driving Lanes - STH 35	169+50 - 209+36	Lower Layer	CIR	5 MT 58-34 V	745	1.25"	PWL Incentive Air Voids Item SPV.0055.02	Incentive Density PWL HMA Pavement Item SPV.0055.01
12-Ft Driving Lanes - STH 35	169+50 - 209+36	Upper Layer	5 MT 58-34 V	5 MT 58-34 V	895	1.5"	PWL Incentive Air Voids Item SPV.0055.02	Incentive Density PWL HMA Pavement Item SPV.0055.01
Shoulders - STH 35	169+50 - 209+36	Lower Layer	CIR	5 MT 58-34 V	380	1.25"	PWL Incentive Air Voids Item SPV.0055.02	Acceptance testing by the Department; Not eligible for incentive
Shoulders - STH 35	169+50-209+36	Upper Layer	5 MT 58-34 V	5 MT 58-34 V	455	1.5"	PWL Incentive Air Voids Item SPV.0055.02	Acceptance testing by the Department; Not eligible for incentive
12-Ft Driving Lanes - STH 35	209+36 - 217+69	Lower Layer	CIR	5 MT 58-34 V	160	1.25"	PWL Incentive Air Voids Item SPV.0055.02	Incentive Density PWL HMA Pavement Item SPV.0055.01
12-Ft Driving Lanes - STH 35	209+36 - 217+69	Upper Layer	5 MT 58-34 V	5 MT 58-34 V	190	1.5"	PWL Incentive Air Voids Item SPV.0055.02	Incentive Density PWL HMA Pavement Item SPV.0055.01
Shoulders, Bypass Lane, Turn Lane, & Side Roads - STH 35	209+36- 217+69	Lower Layer	Milled Existing HMA Surface	5 MT 58-34 V	150	1.25"	PWL Incentive Air Voids Item SPV.0055.02	Acceptance testing by the Department; Not eligible for incentive
Shoulders, Bypass Lane, Turn Lane, & Side Roads - STH 35	209+36 - 217+69	Upper Layer	5 MT 58-34 V	5 MT 58-34 V	175	1.5	PWL Incentive Air Voids Item SPV.0055.02	Acceptance testing by the Department; Not eligible for incentive
12-Ft Driving Lanes - STH 35	217+69 - 238+50	Lower Layer	CIR	5 MT 58-34 V	390	1.25"	PWL Incentive Air Voids Item SPV.0055.02	Incentive Density PWL HMA Pavement Item SPV.0055.01
12-Ft Driving Lanes - STH 35	217+69 - 238+50	Upper Layer	5 MT 58-34 V	5 MT 58-34 V	470	1.5"	PWL Incentive Air Voids Item SPV.0055.02	Incentive Density PWL HMA Pavement Item SPV.0055.01
Shoulders - STH 35	217+69 - 238+50	Lower Layer	CIR	5 MT 58-34 V	200	1.25"	PWL Incentive Air Voids Item SPV.0055.02	Acceptance testing by the Department; Not eligible for incentive
Shoulders - STH 35	217+69 - 238+50	Upper Layer	5 MT 58-34 V	5 MT 58-34 V	240	1.5"	PWL Incentive Air Voids Item SPV.0055.02	Acceptance testing by the Department; Not eligible for incentive
12-Ft Driving Lanes - STH 35	238+50 - 248+30	Lower Layer	CIR	5 MT 58-34 V	185	1.25"	PWL Incentive Air Voids Item SPV.0055.02	Incentive Density PWL HMA Pavement Item SPV.0055.01
12-Ft Driving Lanes - STH 35	238+50 - 248+30	Upper Layer	5 MT 58-34 V	5 MT 58-34 V	220	1.5"	PWL Incentive Air Voids Item SPV.0055.02	Incentive Density PWL HMA Pavement Item SPV.0055.01
Shoulders, Bypass Lane, Turn Lane, & Side Roads - STH 35	238+50 - 248+30	Lower Layer	Milled Existing HMA Surface	5 MT 58-34 V	70	1.25"	PWL Incentive Air Voids Item SPV.0055.02	Acceptance testing by the Department; Not eligible for incentive
Shoulders, Bypass Lane, Turn Lane, & Side Roads - STH 35	238+50 - 248+30	Upper Layer	5 MT 58-34 V	5 MT 58-34 V	75	1.5	PWL Incentive Air Voids Item SPV.0055.02	Acceptance testing by the Department; Not eligible for incentive

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PWL TABLE								
Location	Station	Mixture Use	Underlying Surface	Bid Item	Tons	Thickness	Quality Management Program to be used for:	
							Mixture Acceptance	Density Acceptance
12-Ft Driving Lanes - STH 35	248+30 - 331+25	Lower Layer	CIR	5 MT 58-34 V	1,550	1.25"	PWL Incentive Air Voids Item SPV.0055.02	Incentive Density PWL HMA Pavement Item SPV.0055.01
12-Ft Driving Lanes - STH 35	248+30 - 331+25	Upper Layer	5 MT 58-34 V	5 MT 58-34 V	1,860	1.5"	PWL Incentive Air Voids Item SPV.0055.02	Incentive Density PWL HMA Pavement Item SPV.0055.01
Shoulders - STH 35	248+30 - 331+25	Lower Layer	CIR	5 MT 58-34 V	780	1.25"	PWL Incentive Air Voids Item SPV.0055.02	Acceptance testing by the Department; Not eligible for incentive
Shoulders - STH 35	248+30 - 331+25	Upper Layer	5 MT 58-34 V	5 MT 58-34 V	935	1.5"	PWL Incentive Air Voids Item SPV.0055.02	Acceptance testing by the Department; Not eligible for incentive
12-Ft Driving Lanes - STH 35	331+25 - 352+30	Lower Layer	CIR	5 MT 58-34 V	395	1.25"	PWL Incentive Air Voids Item SPV.0055.02	Incentive Density PWL HMA Pavement Item SPV.0055.01
12-Ft Driving Lanes - STH 35	331+25 - 352+30	Upper Layer	5 MT 58-34 V	5 MT 58-34 V	475	1.5"	PWL Incentive Air Voids Item SPV.0055.02	Incentive Density PWL HMA Pavement Item SPV.0055.01
Shoulders, Bypass Lane, Turn Lane, & Side Roads - STH 35	331+25 - 352+30	Lower Layer	CIR	5 MT 58-34 V	220	1.25"	PWL Incentive Air Voids Item SPV.0055.02	Acceptance testing by the Department; Not eligible for incentive
Shoulders, Bypass Lane, Turn Lane, & Side Roads - STH 35	331+25 - 352+30	Upper Layer	5 MT 58-34 V	5 MT 58-34 V	260	1.5"	PWL Incentive Air Voids Item SPV.0055.02	Acceptance testing by the Department; Not eligible for incentive
12-Ft Driving Lanes - STH 35	352+30 - 358+75	Lower Layer	CIR	5 MT 58-34 V	125	1.25"	PWL Incentive Air Voids Item SPV.0055.02	Incentive Density PWL HMA Pavement Item SPV.0055.01
12-Ft Driving Lanes - STH 35	352+30 - 358+75	Upper Layer	5 MT 58-34 V	5 MT 58-34 V	145	1.5"	PWL Incentive Air Voids Item SPV.0055.02	Incentive Density PWL HMA Pavement Item SPV.0055.01
Shoulders - STH 35	352+30 - 358+75	Lower Layer	CIR	5 MT 58-34 V	60	1.25"	PWL Incentive Air Voids Item SPV.0055.02	Acceptance testing by the Department; Not eligible for incentive
Shoulders - STH 35	352+30 - 358+75	Upper Layer	5 MT 58-34 V	5 MT 58-34 V	70	1.5"	PWL Incentive Air Voids Item SPV.0055.02	Acceptance testing by the Department; Not eligible for incentive
12-Ft Driving Lanes - STH 35	358+75 - 426+08	Lower Layer	Milled Existing HMA Surface	5 MT 58-34 V	1,510	1.5"	PWL Incentive Air Voids Item SPV.0055.02	Incentive Density PWL HMA Pavement Item SPV.0055.01
12-Ft Driving Lanes - STH 35	358+75 - 426+08	Upper Layer	5 MT 58-34 V	5 MT 58-34 V	1,760	1.75"	PWL Incentive Air Voids Item SPV.0055.02	Incentive Density PWL HMA Pavement Item SPV.0055.01
Shoulders, Parking Lanes, & Side Roads - STH 35	358+75 - 426+08	Lower Layer	Milled Existing HMA Surface	5 MT 58-34 V	1,300	1.5"	PWL Incentive Air Voids Item SPV.0055.02	Acceptance testing by the Department; Not eligible for incentive
Shoulders, Parking Lanes, & Side Roads - STH 35	358+75 - 426+08	Upper Layer	5 MT 58-34 V	5 MT 58-34 V	1,730	1.75"	PWL Incentive Air Voids Item SPV.0055.02	Acceptance testing by the Department; Not eligible for incentive
Project		Culvert Patches	Base Aggregate	Asphaltic Surface		6" Total	QMP as per SS 465.	Acceptance by ordinary compaction.
Project	107+00 - 426+08	Patching	Base Aggregate	Asphaltic Surface		Varies	QMP as per SS 460.	Acceptance by ordinary compaction
Project	107+00 - 426+08	Patching	Base Aggregate	Asphaltic Surface Patching		Varies	QMP as per SS 460.	Acceptance by ordinary compaction

- Indicates non PWL.

**305 - 465 DRIVEWAYS**

CATEGORY	STATION	LOCATION	204.0110	305.0110	465.0120	690.0150
			REMOVING ASPHALTIC SURFACE SY	BASE AGGREGATE DENSE 3/4-INCH TON	DRIVEWAYS AND FIELD ENTRANCES TON	SAWING ASPHALT LF
0010	358+09	STH 35 LT	50	-	9	20
0010	334+42	STH 35 LT	-	5	-	-
0010	332+84	STH 35 LT	-	5	-	-
0010	324+25	STH 35 RT	-	5	-	-
0010	323+82	STH 35 RT	50	-	9	27
0010	322+23	STH 35 RT	35	-	6	27
0010	316+08	STH 35 RT	-	5	-	-
0010	302+39	STH 35 LT	40	-	7	12
0010	299+14	STH 35 LT	75	-	13	12
0010	295+77	STH 35 LT	90	-	16	17
0010	290+76	STH 35 RT	45	-	8	10
0010	288+90	STH 35 RT	-	5	-	-
0010	284+00	STH 35 LT	-	5	-	-
0010	281+97	STH 35 LT	-	5	-	-
0010	280+12	STH 35 LT	35	-	6	13
0010	279+41	STH 35 RT	120	5	21	25
0010	276+83	STH 35 LT	-	5	-	-
0010	274+64	STH 35 LT	45	-	8	14
0010	270+55	STH 35 LT	45	-	8	16
0010	267+80	STH 35 LT	-	5	-	-
0010	265+38	STH 35 LT	60	-	11	13
0010	260+04	STH 35 RT	-	5	-	-
0010	258+83	STH 35 RT	-	5	-	-
0010	255+59	STH 35 RT	-	5	-	-
0010	246+79	STH 35 RT	-	5	-	-
0010	240+16	STH 35 RT	-	5	-	-
0010	220+70	STH 35 LT	-	5	-	-
0010	213+65	STH 35 LT	-	5	-	-
0010	182+96	STH 35 RT	-	5	-	-
0010	183+11	STH 35 LT	-	5	-	-
0010	170+24	STH 35 RT	-	5	-	-
0010	157+24	STH 35 RT	-	5	-	-
0010	157+26	STH 35 LT	-	5	-	-
0010	142+67	STH 35 RT	-	5	-	-
0010	142+75	STH 35 LT	-	5	-	-
0010	128+52	STH 35 LT	-	5	-	-
0010	128+61	STH 35 RT	-	5	-	-
0010	116+52	STH 35 LT	-	5	-	-
0010	116+52	STH 35 RT	-	5	-	-
TOTAL 0010			690	140	122	206

\* ADDITIONAL QUANTITIES LISTED ELSEWHERE

**465 ASPHALTIC SURFACE**

CATEGORY	STATION	TO	STATION	LOCATION	* 465.0105 ASPHALTIC SURFACE TON	* 465.0110 ASPHALTIC SURFACE PATCHING TON	REMARKS
					TON	TON	
0010	107+00	-	329+75	STH 35	250	-	CIR SECTION WEDGING AND LEVELING, SPOT LANE FAILURES & EDGE REPAIRS
0010	107+00	-	329+75	STH 35	-	100	CIR SECTION FILLING POT HOLES, TRAFFIC POP OUTS & RAMPING
0010	329+75	-	426+08	STH 35	300	-	MILL/OVERLAY SECTION WEDGING AND LEVELING, SPOT LANE FAILURES & EDGE REPAIRS
0010	329+75	-	426+08	STH 35	-	50	MILL/OVERLAY SECTION FILLING POT HOLES, TRAFFIC POP OUTS & RAMPING
TOTAL 0010					550	150	

\* ADDITIONAL QUANTITIES LISTED ELSEWHERE

**465 RUMBLE STRIPS**

CATEGORY	STATION	TO	STATION	LOCATION	465.0425 ASPHALTIC SHOULDER RUMBLE STRIPS 2-LANE RURAL LF	465.0475 ASPHALT CENTERLINE RUMBLE STRIPS 2-LANE RURAL LF	REMARKS
					LF	LF	
0010	107+00	-	163+50	STH 35	11,300	5,650	TYPE 1
0010	169+50	-	209+61	STH 35 RT	4,011	-	TYPE 1
0010	169+50	-	239+00	STH 35 LT	6,950	-	TYPE 1
0010	169+50	-	211+79	STH 35	-	4,229	---
0010	215+69	-	242+72	STH 35 RT	2,703	-	TYPE 1, TYPE 2 - BYPASS AT CTH M
0010	215+79	-	238+11	STH 35	-	2,232	---
0010	246+14	-	331+75	STH 35 RT	8,561	-	TYPE 1
0010	246+56	-	333+89	STH 35	-	8,733	TYPE 1
0010	248+30	-	343+15	STH 35 LT	9,485	-	---
0010	338+00	-	339+10	STH 35 RT	110	-	---
0010	348+68	-	35900.00	STH 35 LT	1,032	-	---
0010	352+38	-	359+00	STH 35	-	662	TYPE 1
0010	352+94	-	359+00	STH 35 RT	606	-	---
TOTAL 0010					44,758	21,506	

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**CULVERTS**

CATEGORY	LOCATION	203.0100 REMOVING SMALL PIPE CULVERTS EACH	204.9060.S.01 REMOVING (ITEM DESCRIPTION) (01. APRON ENDWALL FOR CPCS 24-INCH) EACH	204.9090.S.01 REMOVING (ITEM DESCRIPTION) (01. CPRC 24-INCH) LF	204.9090.S.02 REMOVING (ITEM DESCRIPTION) (02. CPRC 72- INCH) LF	520.1024 APRON ENDWALLS FOR CULVERT PIPE 24- INCH EACH	520.1036 APRON ENDWALLS FOR CULVERT PIPE 36- INCH EACH	520.2024 CULVERT PIPE TEMPORARY 24- INCH LF	520.2030 CULVERT PIPE TEMPORARY 30- INCH LF	520.3324 CULVERT PIPE CLASS III-A 24- INCH LF	520.4136 CULVERT PIPE CLASS IV 36- INCH LF	520.8700 CLEANING CULVERT PIPES EACH	520.9700.S.01 CULVERT PIPE LINERS (SIZE) (01. 36-INCH) LF	520.9750.S CLEANING CULVERT PIPES FOR LINER VERIFICATION EACH	521.1024 APRON ENDWALLS FOR CULVERT PIPE STEEL 24-INCH EACH
0010	CULVERT 70	-	-	-	-	-	-	-	-	-	-	-	-	-	-
0010	CULVERT 90	-	-	-	8	-	-	-	-	-	-	1	-	-	-
0010	CULVERT 100	-	-	-	-	-	-	-	-	-	-	1	-	-	-
0010	CULVERT 110	-	-	-	-	-	-	-	-	-	-	1	-	-	-
0010	CULVERT 120	-	-	-	-	-	-	-	-	-	-	1	-	-	-
0010	CULVERT 130	-	-	-	-	-	-	-	-	-	-	-	-	-	-
0010	CULVERT 140	-	-	-	-	-	-	-	-	-	-	-	-	-	-
0010	CULVERT 150	-	-	8	-	-	-	-	-	-	-	1	-	-	-
0010	CULVERT 160	-	-	-	-	-	-	-	-	-	-	-	86	1	-
0010	CULVERT 180	1	-	-	-	2	-	8	-	88	-	-	-	-	-
0010	CULVERT 210	1	-	-	-	-	2	-	8	-	56	-	-	-	-
0010	CULVERT 220	-	1	-	-	-	-	-	-	-	-	-	-	-	1
TOTAL 0010		2	1	8	8	2	2	8	8	88	56	5	86	1	1

CATEGORY	LOCATION	522.0124 CULVERT PIPE REINFORCED CONCRETE CLASS III 24-INCH LF	522.0172 CULVERT PIPE REINFORCED CONCRETE CLASS III 72-INCH LF	522.1024 APRON ENDWALLS FOR CULVERT PIPE REINFORCED CONCRETE 24- INCH EACH	522.1030 APRON ENDWALLS FOR CULVERT PIPE REINFORCED CONCRETE 30- INCH EACH	522.1054 APRON ENDWALLS FOR CULVERT PIPE REINFORCED CONCRETE 54- INCH EACH	522.1072 APRON ENDWALLS FOR CULVERT PIPE REINFORCED CONCRETE 72- INCH EACH	606.0200 RIPRAP MEDIUM CY	606.0400 RIPRAP EXTRA- HEAVY CY	611.9800.S PIPE GRATES EACH	633.5200 MARKERS CULVERT END EACH	645.0120 GEOTEXTILE TYPE HR SY	SPV.0060.06 SPECIAL (06. GRADING AND SHAPING APRON ENDWALL INSTALLATION) EACH	SPV.0090.01 SPECIAL (01. DITCH CLEANING) LF	REMARKS
0010	CULVERT 70	-	-	-	-	2	-	5	-	-	2	15	2	-	EXISTING 54-INCH CPRC
0010	CULVERT 90	-	8	-	-	-	2	-	-	-	2	-	2	150	EXISTING 72-INCH CPRC
0010	CULVERT 100	-	-	-	2	-	-	4	-	-	2	11	2	-	EXISTING 30-INCH CPRC
0010	CULVERT 110	-	-	2	-	-	-	2	-	-	2	6	2	400	EXISTING 24-INCH CPRC
0010	CULVERT 120	-	-	-	-	-	2	10	-	-	2	25	2	-	EXISTING 72-INCH CPRC
0010	CULVERT 130	-	-	2	-	-	-	-	-	-	2	-	2	200	EXISTING 24-INCH CPRC
0010	CULVERT 140	-	-	-	-	-	2	-	-	-	2	-	2	-	EXISTING 72-INCH CPRC
0010	CULVERT 150	8	-	2	-	-	-	-	-	1	2	-	2	325	EXISTING 24-INCH CPRC
0010	CULVERT 160	-	-	-	-	-	-	4	-	-	2	13	-	-	EXISTING 36-INCH CMCP
0010	CULVERT 180	-	-	-	-	-	-	-	4	-	2	12	-	-	EXISTING 24-INCH CMCP
0010	CULVERT 210	-	-	-	-	-	-	-	7	-	2	18	-	-	EXISTING 30-INCH CMCP
0010	CULVERT 220	-	-	-	-	-	-	-	-	-	2	-	1	-	EXISTING 24-INCH CMCP
TOTAL 0010		8	8	6	2	2	6	25	11	1	24	100	17	1,075	

NOTE: ALL CPRC SHALL BE TIED TO EXISTING SECTIONS AND TO NEW ENDWALLS.





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**611 INLET COVERS**

CATEGORY	LOCATION	*			
		611.8110 ADJUSTING MANHOLE COVERS EACH	611.8115 ADJUSTING INLET COVERS EACH	611.8120.S COVER PLATES TEMPORARY EACH	SPV.0060. SPECIAL (08. INLET COVERS, TYPE H-D) EACH
0010	COLLEGE NE	-	1	-	-
0010	COLLEGE NW	-	1	-	-
0010	CHASE NE	-	2	-	-
0010	CHASE NW	-	1	-	-
0010	SUMNER NE	-	1	-	-
0010	SUMNER NW	-	1	-	-
0010	KING NE	-	2	-	-
0010	KING NW	-	-	-	1
0010	JAY NE	-	2	-	-
0010	JAY NW	-	1	-	-
0010	GRAY NE	-	1	-	-
0010	GRAY NW	-	1	-	-
0010	SOUTH NE	-	2	-	-
0010	SOUTH NW	-	1	-	-
0010	MAIN NE	-	2	-	-
0010	MAIN NW	-	1	-	1
0010	MAIN SE	-	-	-	1
0010	MAIN SW	-	-	-	1
0010	STH 35	10	-	10	-
TOTAL 0010		10	20	10	4

\* ADDITIONAL QUANTITIES LISTED ELSEWHERE

**624.0100 - WATER**

CATEGORY	STATION	TO	STATION	LOCATION	624.0100 WATER MGAL	REMARKS
0010	312+06	-	313+21	35	7	AT CULVERT REPLACEMENTS
0010	326+14	-	327+31	35	7	AT CULVERT REPLACEMENTS
0010	169+50	-	209+36	35	19	SHOULDERING
0010	209+36	-	217+69	35	1	SHOULDERING
0010	217+69	-	238+50	35	10	SHOULDERING
0010	238+50	-	248+30	35	1	SHOULDERING
0010	248+30	-	331+25	35	39	SHOULDERING
0010	331+25	-	352+30	35	1	SHOULDERING
0010	352+30	-	358+75	35	4	SHOULDERING
TOTAL 0010					89	

**628 EC MOBILIZATIONS**

CATEGORY	STATION	TO	STATION	LOCATION	628.1905	628.1910
					MOBILIZATIONS EROSION CONTROL EACH	MOBILIZATIONS EMERGENCY EROSION CONTROL EACH
0010	107+00	-	163+50	STH 35	4	2
TOTAL 0010					4	2

**625-630 EROSION CONTROL**

CATEGORY	STATION	TO	STATION	LOCATION	625.0100	628.1504	628.1520	628.2004	628.2006	628.7005	628.7015	628.7504	628.7555	628.7570	629.0210	630.0110	630.0140	630.0200	630.0500	REMARKS
					TOPSOIL SY	SILT FENCE LF	SILT FENCE MAINTENANCE LF	EROSION MAT CLASS I TYPE B SY	EROSION MAT URBAN CLASS I TYPE A SY	INLET PROTECTION TYPE A EACH	INLET PROTECTION TYPE C EACH	TEMPORARY DITCH CHECKS LF	CULVERT PIPE CHECKS EACH	ROCK BAGS EACH	FERTILIZER TYPE B CWT	SEEDING MIXTURE NO. 10 LB	SEEDING MIXTURE NO. 40 LB	SEEDING TEMPORARY LB	SEED WATER MGAL	
0010	107+00	-	163+50	STH 35	-	4,105	4,105	-	-	-	-	-	-	-	-	-	-	-	-	
0010	186+75	-	188+25	STH 35	445	-	-	445	-	-	-	-	1	42	0.3	7	-	1.8	8	CULVERT 70
0010	211+50	-	212+50	STH 35	335	-	-	335	-	-	-	24	1	15	0.3	5	-	1.3	6	CULVERT 90
0010	219+00	-	220+00	STH 35	400	-	-	400	-	-	-	-	1	5	0.3	6	-	1.5	7	CULVERT 100
0010	223+50	-	225+50	STH 35	890	-	-	890	-	-	-	48	1	3	0.6	13	-	3.3	15	CULVERT 110
0010	228+25	-	229+25	STH 35	410	-	-	410	-	-	-	-	1	15	0.3	6	-	1.50	7	CULVERT 120
0010	230+00	-	232+50	STH 35	270	-	-	270	-	-	-	24	1	3	0.2	4	-	1.00	5	CULVERT 130
0010	255+25	-	256+00	STH 35	25	-	-	25	-	-	-	-	1	15	0.1	1	-	0.30	1	CULVERT 140
0010	260+00	-	262+00	STH 35	445	-	-	445	-	-	-	48	1	3	0.3	7	-	1.80	8	CULVERT 150
0010	273+00	-	273+75	STH 35	225	-	-	225	-	-	-	-	1	7	0.2	4	-	1.0	4	CULVERT 160
0010	312+00	-	312+21	STH 35	635	-	-	635	-	-	-	-	2	11	0.5	9	-	2.3	11	CULVERT 180
0010	326+14	-	327+31	STH 35	575	-	-	575	-	-	-	-	1	5	0.4	8	-	2.0	10	CULVERT 210
0010	326+14	-	327+31	STH 35	-	2,500	2,500	-	-	-	-	-	-	-	0.0	-	-	-	-	CULVERT 210
0010	339+15	-	339+40	STH 35	25	-	-	25	-	-	-	-	1	3	0.1	1	-	0.30	1	CULVERT 220
0010	360+00	-	426+08	STH 35	475	-	-	-	475	4	33	-	-	-	-	-	7	1.80	8	URBAN SECTION
UNDISTRIBUTED (25%)					1,295	1,670	1,670	1,170	125	1	12	56	4	48	0.9	19	3	5.1	24	
TOTAL 0010					6,450	8,275	8,275	5,850	600	5	45	200	17	175	4.5	90	10	25	115	

3

PROJECT NO: 7140-00-70

HWY: STH 35

COUNTY: TREMPEALEAU

MISCELLANEOUS QUANTITIES

SHEET:

E

MOVING SIGNS

CATEGORY	STATION	TO	STATION	LOCATION	634.0618 POSTS WOOD 4X6-INCH X 18- FT EACH	638.2102 MOVING SIGNS TYPE II EACH	638.3000 REMOVING SMALL SIGN SUPPORTS EACH	638.4000 MOVING SMALL SIGN SUPPORTS EACH
0010	107+00	-	426+08	STH 35	2	11	1	8
TOTAL 0010					2	11	1	8

POSTS INCLUDED FOR NON SALVAGABLE POSTS

643 TRAFFIC CONTROL

CATEGORY	STATION	TO	STATION	LOCATION	DURATION DAYS	EACH**	DRUMS DAY	EACH**	TYPE III BARRICADES DAY	EACH**	TYPE A LIGHTS DAY	EACH**	TYPE C LIGHTS DAY	EACH**	TRAFFIC CONTROL SIGNS DAY	643.1000 TRAFFIC CONTROL SIGNS FIXED MESSAGE SF	REMARKS
0010	PROJECT			STH 35	7	-	-	-	-	-	-	-	-	-	-	64	SIGNING PRIOR TO WORK BEGINNING
0010	PROJECT			STH 35	104	-	-	-	-	-	-	-	67	6,968	-	-	ADVANCED WIDTH RESTRICTION & ADVANCED WARNING SIGNING
0010	358+00	-	426+08	STH 35	30	175	5,250	6	180	12	360	9	270	6	180	-	CURB RAMP REPLACEMENTS/SHOULDER CLOSURE/UTILITY WORK
0010	312+00	-	328+00	STH 35	2	25	50	-	-	-	-	-	-	-	-	-	CULVERT REPLACEMENTS
0010	180+00	-	350+00	STH 35	14	150	2,100	-	-	-	-	-	-	-	-	-	SHOULDER CLOSURES
0010	107+00	-	331+75	STH 35	28	225	6,300	-	-	-	-	-	-	-	-	-	CIR (DRUMS FOR SHOULDER CLOSURE UNTIL SURFACE LAYERS)
0010	PROJECT			UNDISTRIBUTED (10%)			1,400		20		40		30		752		
TOTAL 0010							15,100		200		400		300		7,900	64	

\* ADDITIONAL QUANTITIES LISTED ELSEWHERE

\*\*FOR INFORMATION ONLY

643-644 TEMP PED ACCOMMODATIONS

CATEGORY	STATION	TO	STATION	LOCATION	DURATION DAYS	EACH**	TRAFFIC CONTROL SIGNS DAY	EACH**	TEMPORARY PEDESTRIAN CURB RAMP DAY	TEMPORARY PEDESTRIAN BARRICADE LF	SPV.0090.03 SPECIAL (03. TEMPORARY MARKING LINE REMOVABLE TAPE 6-INCH) LF	644.1605 TEMPORARY PEDESTRIAN DETECTABLE WARNING FIELD SF	644.1440 TEMPORARY PEDESTRIAN SURFACE MATTING SF
0010	360+00	-	420+00	STH 35	21	37	777	37	777	4,995	900	370	925
0010	PROJECT			UNDISTRIBUTED (10%)			123		123	505	100	40	100
TOTAL 0010							900		900	5,500	1,000	410	1,025

\* ADDITIONAL QUANTITIES LISTED ELSEWHERE

\*\*FOR INFORMATION ONLY

**PAVEMENT MARKING**

CATEGORY	STATION	TO	STATION	LOCATION	646.1020		646.1040	646.3040	646.4520	646.5020	646.5120	646.5220	646.6120	646.7120	646.7420	646.8120	646.8320	643.3120	643.3105	REMARKS
					MARKING LINE EPOXY 4-INCH YELLOW LF	MARKING LINE EPOXY 4-INCH WHITE LF	MARKING LINE GROOVED WET REF EPOXY 4-INCH LF	MARKING LINE GROOVED WET REF EPOXY 8-INCH LF	MARKING LINE SAME DAY EPOXY 4-INCH LF	MARKING ARROW EPOXY EACH	MARKING WORD EPOXY EACH	MARKING SYMBOL EPOXY EACH	MARKING STOP LINE EPOXY 18-INCH LF	MARKING DIAGONAL EPOXY 12-INCH LF	MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH LF	MARKING CURB EPOXY LF	MARKING PARKING STALL EPOXY LF	TEMPORARY MARKING LINE EPOXY 4-INCH LF	TEMPORARY MARKING LINE PAINT 4-INCH LF	
0010	107+00	-	426+08	PROJECT	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
0010	107+00	-	163+50	STH 35	1,412.5	-	11,300	-	-	-	-	-	-	-	-	-	-	904	1,413	
0010	169+50	-	208+25	STH 35	975	-	7,750	-	-	-	-	-	-	-	-	-	-	620	969	
0010	208+25	-	218+75	STH 35/CTH M	1,253	-	1,975	195	-	-	-	-	-	-	-	-	-	2,018	1,188	CTH M INTERSECTION
0010	218+75	-	237+50	STH 35	825	-	3,750	-	-	-	-	-	-	-	-	-	-	300	938	
0010	237+50	-	249+50	STH 35/SOUTH ST	875	-	2,202	-	-	-	-	-	-	-	-	-	-	1,192	800	SOUTH ST INTERSECTION
0010	249+50	-	329+75	STH 35	2,612.5	-	16,050	-	-	-	-	-	-	-	-	-	-	2,484	2,607	
0010	329+75	-	359+00	STH 35	5,378	-	5,590	957	-	2	1	-	-	325	-	-	-	3,515	3,788	BEMIS/EMMONS INTERSECTIONS
0010	359+00	-	426+08	STH 35 (URBAN)	-	11,465	-	-	7,035	-	-	-	20	-	417	-	-	11,518	-	
				SUBTOTAL	13,331	11,465	48,617	1,152	7,035	2	1	-	20	325	417	-	-	22,551	11,703	
				TOTAL 0010	24,796		48,617	1,152	7,035	2	1	0	20	325	417	0	0	22,551	11,703	
0020	358+75	-	425+82	STH 35 PARKING LANES	-	-	-	-	-	-	-	1	-	-	-	2,000	306	-	-	
				TOTAL 0020	0	0	0	0	0	0	0	1	0	0	0	2,000	306	0	0	
				PROJECT TOTAL	24,796		48,617	1,152	7,035	2	1	1	20	325	417	2,000	306	22,551	11,703	

\* ADDITIONAL QUANTITIES LISTED ELSEWHERE

CIR/RURAL SECTION

- 1) PLACE TEMPORARY MARKING LINE PAINT ON SURFACE OF CIR LAYER AND LOWER LAYER.
- 2) PLACE TEMPORARY MARKING LINE EPOXY ON TOP LAYER AND PRIOR TO CL RUMBLE STRIPS.
- 3) PLACE MARKING LINE EPOXY ON TOP LAYER AFTER CL RUMBLE STRIPS.

MILL & OVERLAY/URBAN SECTION

- 1) PLACE TEMPORARY MARKING LINE PAINT ON MILLED SURFACE AND LOWER LAYER.
- 2) PLACE MARKING LINE SAME DAY EPOXY ON TOP LAYER.

**643.0920 - COVERING SIGNS**

CATEGORY	STATION	TO	STATION	LOCATION	646.0920		REMARKS
					TRAFFIC CONTROL COVERING SIGNS NO SIGNS*	TYPE II CYCLES* EACH	
STAGE 1	0010	358+00	426+08	STH 35/SIDE ROADS	12	1	12
				TOTAL 0010			12

\*FOR INFORMATION ONLY

**643-646 TEMP MARKING LINE**

CATEGORY	STATION	TO	STATION	LOCATION	646.9000	643.3105	REMARKS
					MARKING REMOVAL LINE 4-INCH LF	TEMPORARY MARKING LINE PAINT 4-INCH LF	
STAGE 1	0010	354+00	422+00	STH 35	12,100	6,300	
STAGE 2	0010	354+00	422+00		0	7,035	REPLACE EXISTING CL
				TOTAL 0010	12,100	13,335	

\* ADDITIONAL QUANTITIES LISTED ELSEWHERE

3

3

650 STAKING

CATEGORY	STATION	TO	STATION	LOCATION	650.6000 CONSTRUCTION STAKING PIPE CULVERTS EACH	650.8000 CONSTRUCTION STAKING RESURFACING REFERENCE LF	650.8501.01 CONSTRUCTION STAKING ELECTRICAL INSTALLATIONS (PROJECT) (01. 7140-00-70) EACH	650.9911.01 CONSTRUCTION STAKING SUPPLEMENTAL CONTROL (PROJECT) (01. 7140-00-70) EACH	650.9500.01 CONSTRUCTION STAKING SIDEWALK (PROJECT) (01. 7140-00-70) EACH
0010	107+00	-	426+08	STH 35	2	31,908	1	1	1
TOTAL 0010					2	31,908	1	1	1

652 - 655 ELECTRICAL

CATEGORY	STATION	TO	STATION	LOCATION	204.0195 REMOVING CONCRETE BASES EACH	652.0215 CONDUIT RIGID NONMETALLIC SCHEDULE 40 1 1/4- INCH LF	654.0105 CONCRETE BASES TYPE 5 EACH	655.0610 ELECTRICAL WIRE LIGHTING 12 AWG LF	655.0625 ELECTRICAL WIRE LIGHTING 6 AWG LF	SPV.0060.03 SPECIAL (03. RELOCATE EXISTING LIGHT POLE ) EACH	REMARKS
0010	402+25	-	402+75	STH 35 LT	2	30	2	250	500	2	MAIN ST/3RD ST INTERSECTION
TOTAL 0010					2	30	2	250	500	2	

UTILITY WORK

CATEGORY	STATION	TO	STATION	LOCATION	* 204.0110 REMOVING ASPHALTIC SURFACE SY	* 305.0120 BASE AGGREGATE DENSE 1 1/4- INCH TON	* 465.0110 ASPHALTIC SURFACE PATCHING TON	* 611.0530 MANHOLE COVERS TYPE J EACH	* 611.8120.S COVER PLATES TEMPORARY EACH	* 690.0150 SAWING ASPHALT LF	SPV.0060.01 SPECIAL (01. 6- INCH GATE VALVE AND BOX) EACH	SPV.0060.02 SPECIAL (02. HYDRANT) EACH	SPV.0060.04 SPECIAL (04. ADJUSTING WATER VALVE BOX) EACH	SPV.0060.05 SPECIAL (05. ADJUSTING SANITARY MANHOLE COVER) EACH	REMARKS
0010	369+00	-	426+08	STH 35	9	-	3	6	6	72	-	-	-	-	REPLACE STORM SEWER MANHOLE COVERS
TOTAL 0010					9	-	3	6	6	72	-	-	-	-	
0020	361+50	-	426+08	STH 35	100	44	26	-	-	360	9	-	-	-	WATER VALVE REPLACEMENTS
0020	361+50	-	426+08	STH 35	-	-	-	-	-	-	-	2	-	-	HYDRANT REPLACEMENTS
0020	361+50	-	426+08	STH 35	-	-	-	-	17	-	-	-	11	17	PROJECT ADJUSTMENTS
TOTAL 0020					100	44	26	-	17	360	9	2	11	17	
PROJECT TOTAL					109	44	29	6	23	432	9	2	11	17	

\* ADDITIONAL QUANTITIES LISTED ELSEWHERE



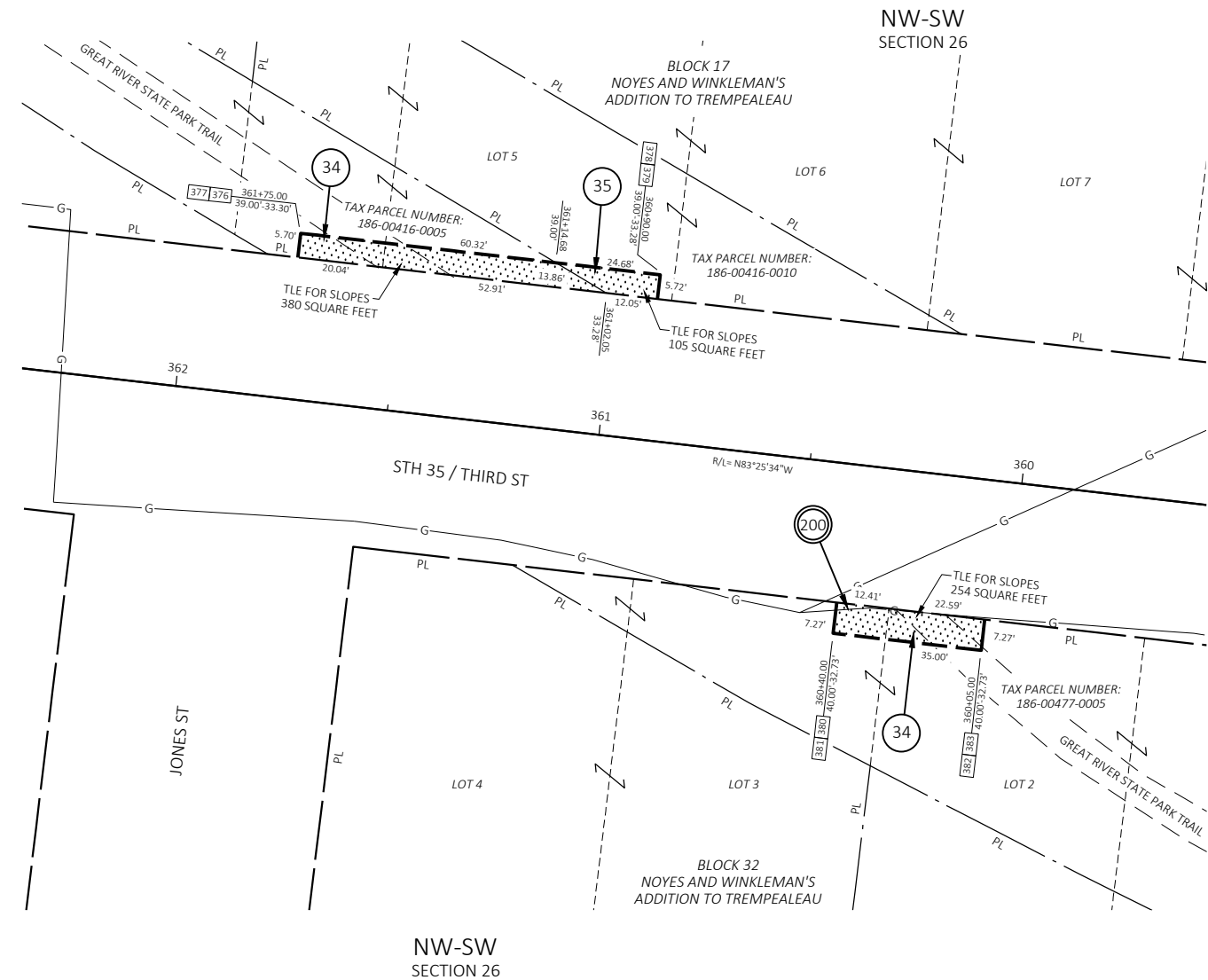
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R/W PROJECT NUMBER: 7140-00-20 SHEET NUMBER: 1

TLE ACQUISITION EXHIBIT  
LA CROSSE - TREMPPEALEAU  
LA CROSSE / TREMPPEALEAU COUNTY LINE TO 10TH ST  
TREMPEALEAU COUNTY  
THAT PART OF LOT 5, BLOCK 17 AND PART OF LOTS 2 THRU 3, BLOCK 32 OF NOYES AND WINKLEMAN'S ADDITION TO TREMPPEALEAU, LOCATED IN AND INCLUDING PART OF THE NW 1/4 - SW 1/4, SECTION 26, T18N, R9W, VILLAGE OF TREMPPEALEAU, TREMPPEALEAU COUNTY, WISCONSIN.



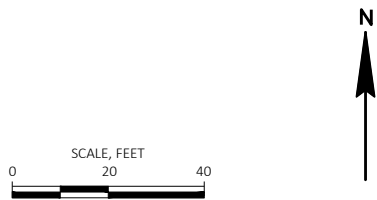
**SCHEDULE OF LANDS & INTERESTS REQUIRED**

OWNERS NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE DEPARTMENT

PARCEL NUMBER	OWNER(S)	INTEREST REQUIRED	TLE SQUARE FEET
34	DEPT OF NATURAL RESOURCES	TLE	634
35	VILLAGE OF TREMPPEALEAU	TLE	105

**UTILITY INTERESTS REQUIRED**

UTILITY NUMBER	UTILITY OWNER(S)	INTEREST REQUIRED
200	MIDWEST NATURAL GAS	NO RECORD OF EASEMENT



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R/W PROJECT NUMBER: 7140-00-20 SHEET NUMBER: 2

TLE ACQUISITION EXHIBIT  
LA CROSSE - TREMPPEALEAU  
LA CROSSE / TREMPPEALEAU COUNTY LINE TO 10TH ST  
TREMPEALEAU COUNTY  
THAT PART OF LOT 6, BLOCK 19 AND PART OF LOT 10, BLOCK 20 OF NOYES AND WINKLEMAN'S ADDITION TO TREMPPEALEAU, NW 1/4 - SW 1/4, SECTION 26, T18N, R9W, VILLAGE OF TREMPPEALEAU, TREMPPEALEAU COUNTY, WISCONSIN.



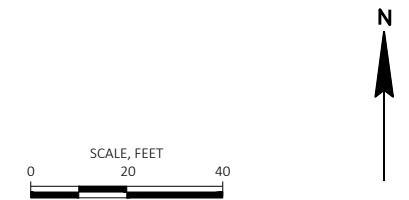
**SCHEDULE OF LANDS & INTERESTS REQUIRED**

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PARCEL NUMBER	OWNER(S)	INTEREST REQUIRED	TLE SQUARE FEET
36	ADAM K JOHNSON	TLE	82
37	ANDREW J MATUSKA	TLE	38

**UTILITY INTERESTS REQUIRED**

UTILITY NUMBER	UTILITY OWNER(S)	INTEREST REQUIRED
200	MIDWEST NATURAL GAS	NO RECORD OF EASEMENT



NOTES:

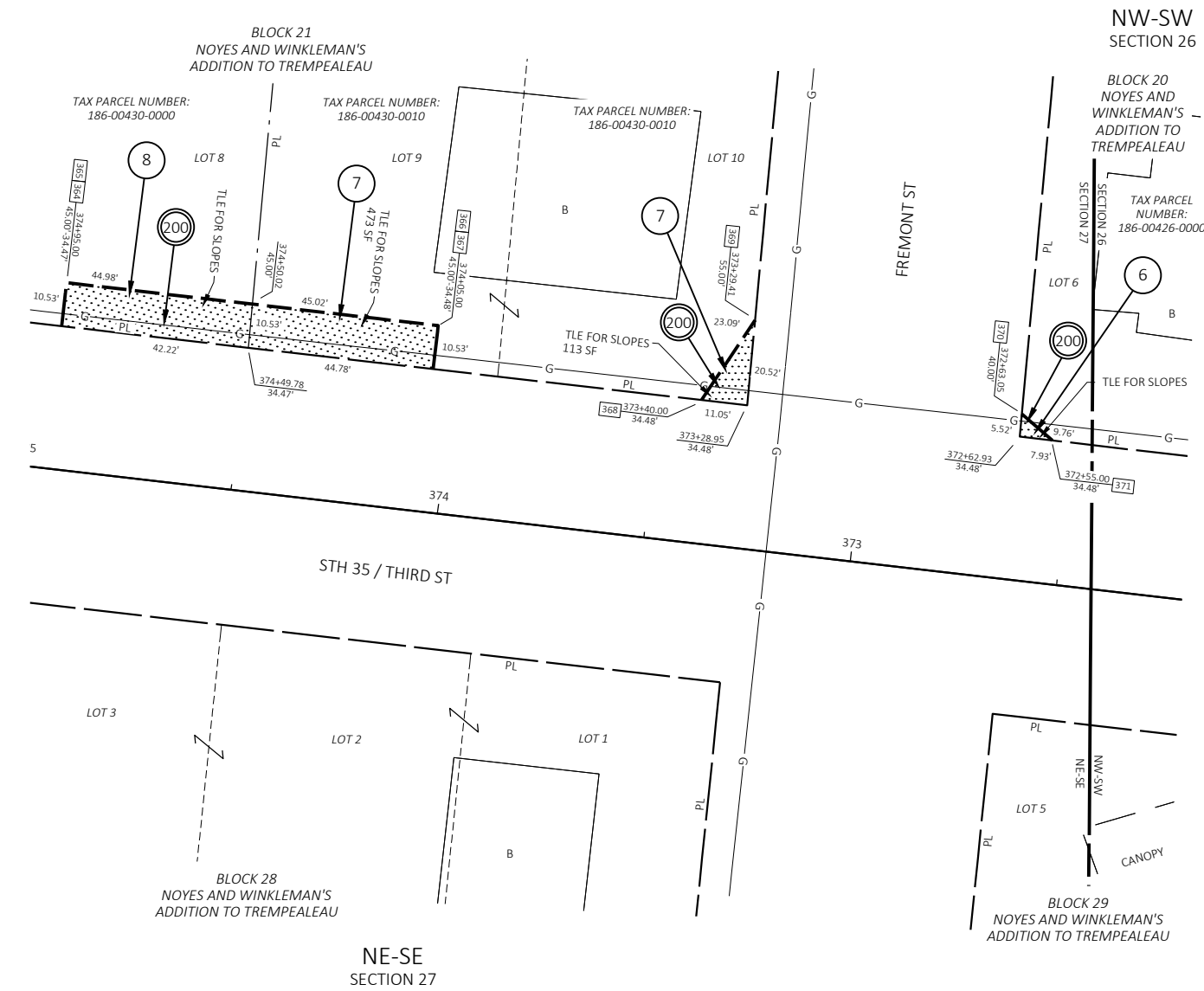
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R/W PROJECT NUMBER: 7140-00-20 SHEET NUMBER: 3

TLE ACQUISITION EXHIBIT  
LA CROSSE - TREMPPEALEAU  
LA CROSSE / TREMPPEALEAU COUNTY LINE TO 10TH ST  
TREMPEALEAU COUNTY  
STH 35



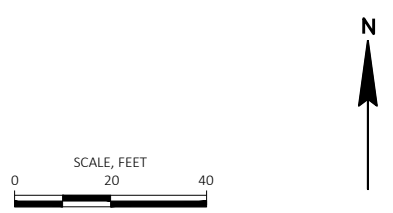
**SCHEDULE OF LANDS & INTERESTS REQUIRED**

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PARCEL NUMBER	OWNER(S)	INTEREST REQUIRED	TLE SQUARE FEET
6	BROOKS K HOFFMANN AND JENNIFER E MCDONNELL	TLE	22
7	STEVEN AND LOIS ZUELKE	TLE	586
8	CITIZENS STATE BANK	TLE	475

**UTILITY INTERESTS REQUIRED**

UTILITY NUMBER	UTILITY OWNER(S)	INTEREST REQUIRED
200	MIDWEST NATURAL GAS	NO RECORD OF EASEMENT



NOTES:

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R/W PROJECT NUMBER: 7140-00-20 SHEET NUMBER: 4

TLE ACQUISITION EXHIBIT  
LA CROSSE - TREMPPEALEAU  
LA CROSSE / TREMPPEALEAU COUNTY LINE TO 10TH ST  
TREMPEALEAU COUNTY  
STH 35



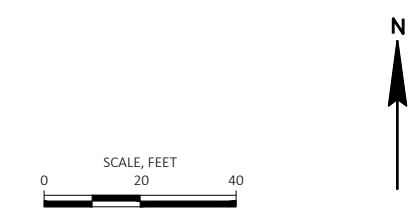
**SCHEDULE OF LANDS & INTERESTS REQUIRED**

OWNERS NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE DEPARTMENT

PARCEL NUMBER	OWNER(S)	INTEREST REQUIRED	TLE SQUARE FEET
8	CITIZENS STATE BANK	TLE	55
9	HAYES A JOHNSON	TLE	123

**UTILITY INTERESTS REQUIRED**

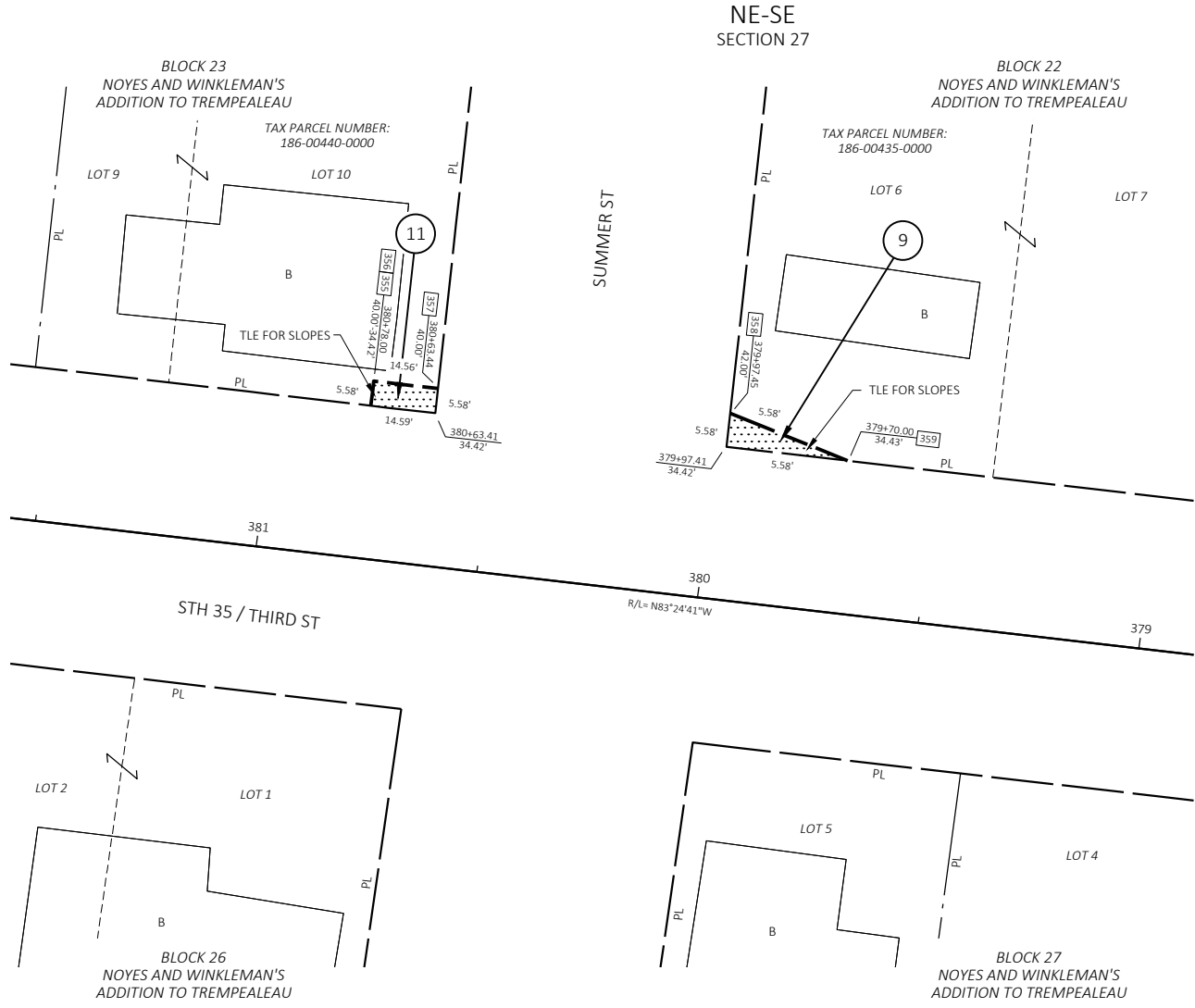
UTILITY NUMBER	UTILITY OWNER(S)	INTEREST REQUIRED
N/A	N/A	N/A





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R/W PROJECT NUMBER: 7140-00-20 SHEET NUMBER: 5  
 TLE ACQUISITION EXHIBIT  
 LA CROSSE - TREMPPEALEAU  
 LA CROSSE / TREMPPEALEAU COUNTY LINE TO 10TH ST  
 TREPPEALEAU COUNTY  
 THAT PART OF LOT 6, BLOCK 22 AND PART OF LOT 10, BLOCK 23 OF NOYES AND WINKLEMAN'S ADDITION TO TREMPPEALEAU, NE 1/4 - SE 1/4, SECTION 27, T18N, R9W, VILLAGE OF TREMPPEALEAU, TREMPPEALEAU COUNTY, WISCONSIN.



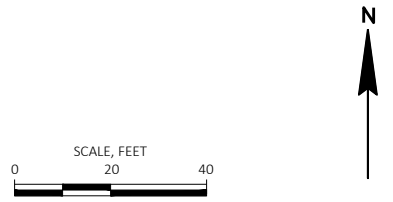
**SCHEDULE OF LANDS & INTERESTS REQUIRED**

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PARCEL NUMBER	OWNER(S)	INTEREST REQUIRED	TLE SQUARE FEET
9	HAYES A JOHNSON	TLE	104
11	JOHN C MARTLAND	TLE	81

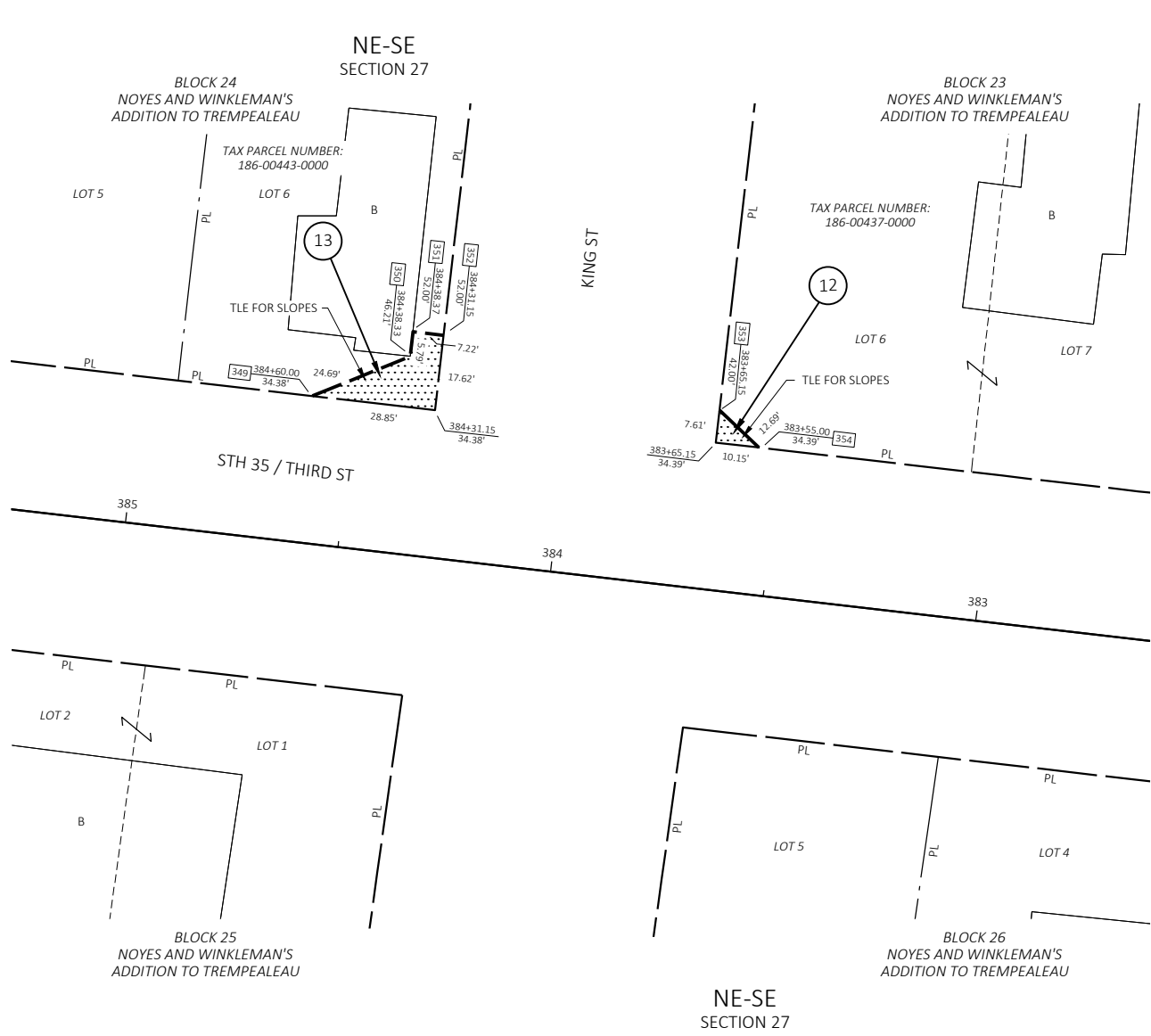
**UTILITY INTERESTS REQUIRED**

UTILITY NUMBER	UTILITY OWNER(S)	INTEREST REQUIRED
N/A	N/A	N/A



NOTES:  
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R/W PROJECT NUMBER: 7140-00-20 SHEET NUMBER: 6  
 TLE ACQUISITION EXHIBIT  
 LA CROSSE - TREMPPEALEAU  
 LA CROSSE / TREMPPEALEAU COUNTY LINE TO 10TH ST  
 TREPPEALEAU COUNTY  
 THAT PART OF LOT 6, BLOCK 23 AND PART OF LOT 6, BLOCK 24 OF NOYES AND WINKLEMAN'S ADDITION TO TREMPPEALEAU, NE 1/4 - SE 1/4, SECTION 27, T18N, R9W, VILLAGE OF TREMPPEALEAU, TREMPPEALEAU COUNTY, WISCONSIN.



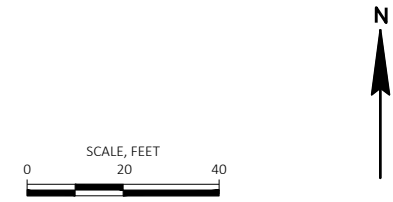
**SCHEDULE OF LANDS & INTERESTS REQUIRED**

OWNERS NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE DEPARTMENT

PARCEL NUMBER	OWNER(S)	INTEREST REQUIRED	TLE SQUARE FEET
12	ROBERT J AND JOYCE M RAND	TLE	39
13	JONATHAN D AND MELISSA L BRYANT	TLE	255

**UTILITY INTERESTS REQUIRED**

UTILITY NUMBER	UTILITY OWNER(S)	INTEREST REQUIRED
N/A	N/A	N/A



NOTES:

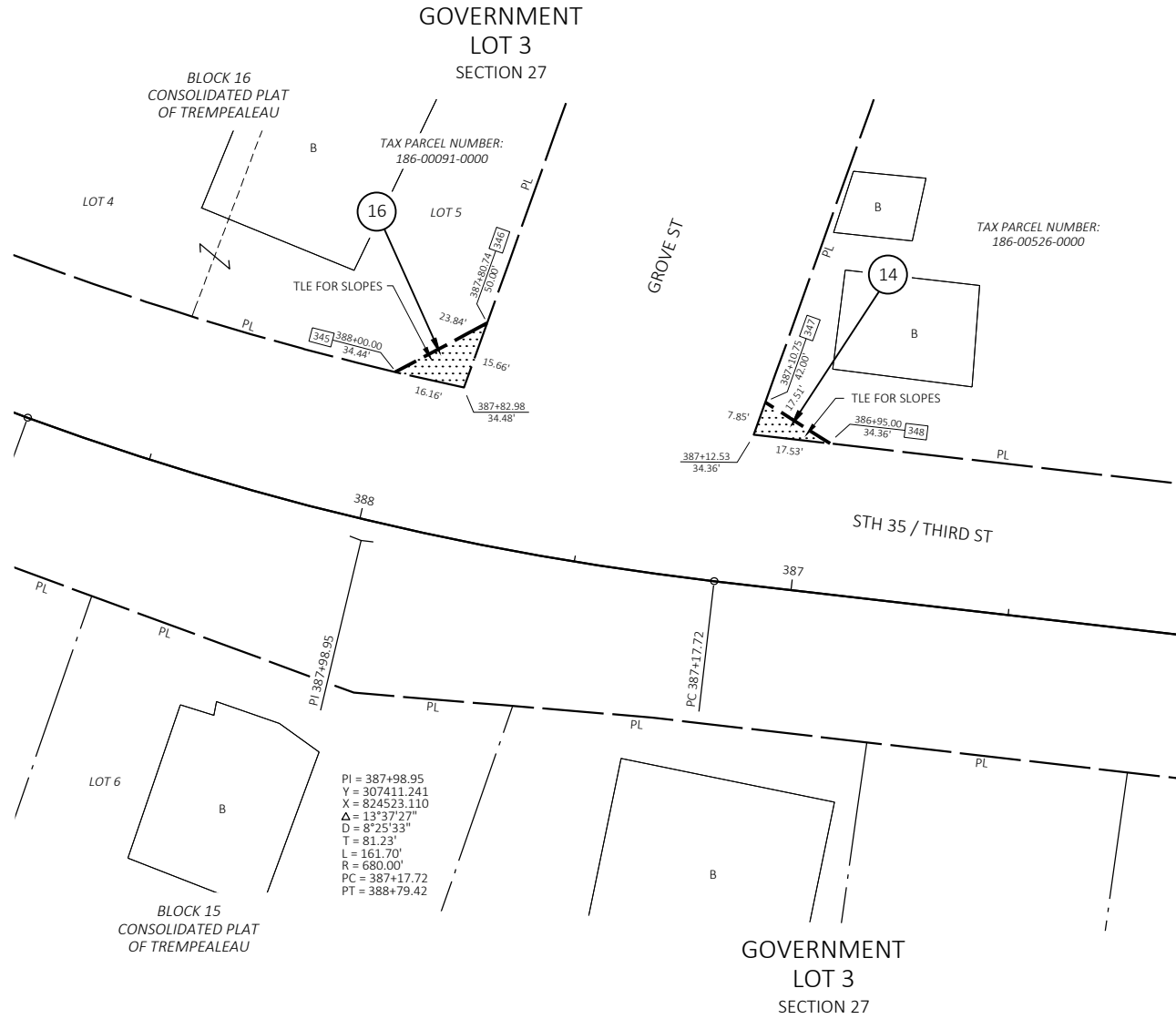
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R/W PROJECT NUMBER: 7140-00-20 SHEET NUMBER: 7

TLE ACQUISITION EXHIBIT  
LA CROSSE - TREMPPEALEAU  
LA CROSSE / TREMPPEALEAU COUNTY LINE TO 10TH ST  
TREMPEALEAU COUNTY  
THAT PART OF LOT 5, BLOCK 16 OF THE CONSOLIDATED PLAT OF TREMPPEALEAU, LOCATED IN AND INCLUDING PART OF GOVERNMENT LOT 3, SECTION 27, T18N, R9W, VILLAGE OF TREMPPEALEAU, TREMPPEALEAU COUNTY, WISCONSIN.



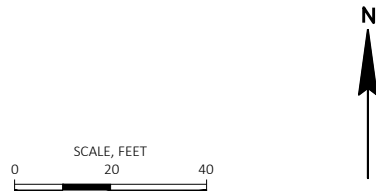
SCHEDULE OF LANDS & INTERESTS REQUIRED

OWNERS NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE DEPARTMENT

PARCEL NUMBER	OWNER(S)	INTEREST REQUIRED	TLE SQUARE FEET
14	VERNONA AND KAREN D KLINGBEIL	TLE	67
16	MICHAEL H, JAMES D AND MONICA M KNEPPER AND ELIZABETH A MULLER	TLE	126

UTILITY INTERESTS REQUIRED

UTILITY NUMBER	UTILITY OWNER(S)	INTEREST REQUIRED
N/A	N/A	N/A



NOTES:

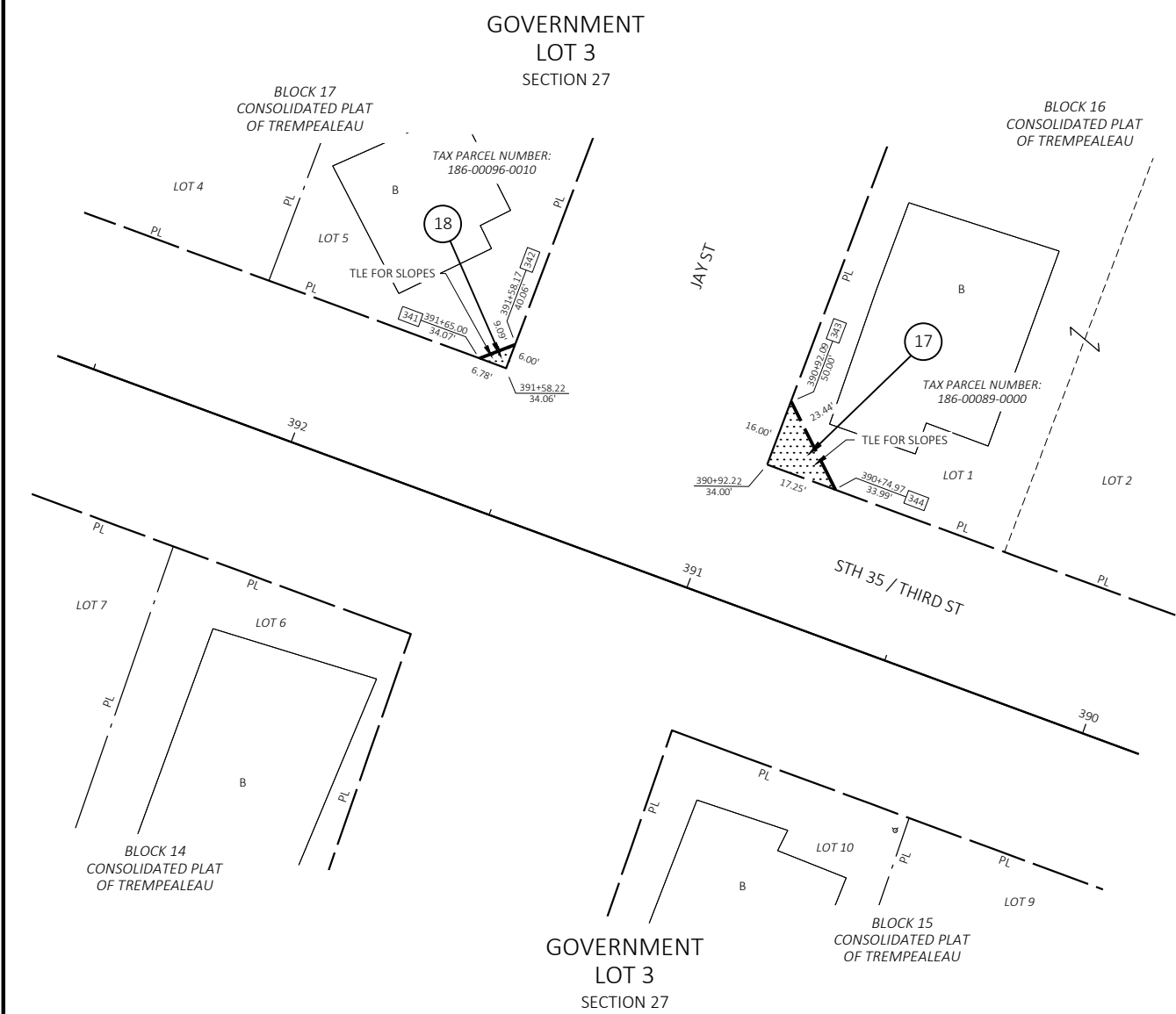
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R/W PROJECT NUMBER: 7140-00-20 SHEET NUMBER: 8

TLE ACQUISITION EXHIBIT  
LA CROSSE - TREMPPEALEAU  
LA CROSSE / TREMPPEALEAU COUNTY LINE TO 10TH ST  
TREMPEALEAU COUNTY  
THAT PART OF LOT 1, BLOCK 16 AND PART OF LOT 5, BLOCK 17 OF THE CONSOLIDATED PLAT OF TREMPPEALEAU, GOVERNMENT LOT 3, SECTION 27, T18N, R9W, VILLAGE OF TREMPPEALEAU, TREMPPEALEAU COUNTY, WISCONSIN.



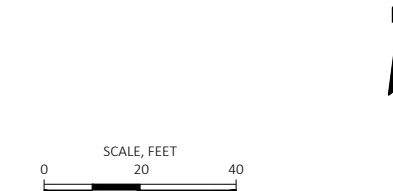
SCHEDULE OF LANDS & INTERESTS REQUIRED

OWNERS NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE DEPARTMENT

PARCEL NUMBER	OWNER(S)	INTEREST REQUIRED	TLE SQUARE FEET
17	SARA L GALLUP	TLE	138
18	KATELIN ELIZABETH SONSALLA	TLE	20

UTILITY INTERESTS REQUIRED

UTILITY NUMBER	UTILITY OWNER(S)	INTEREST REQUIRED
N/A	N/A	N/A



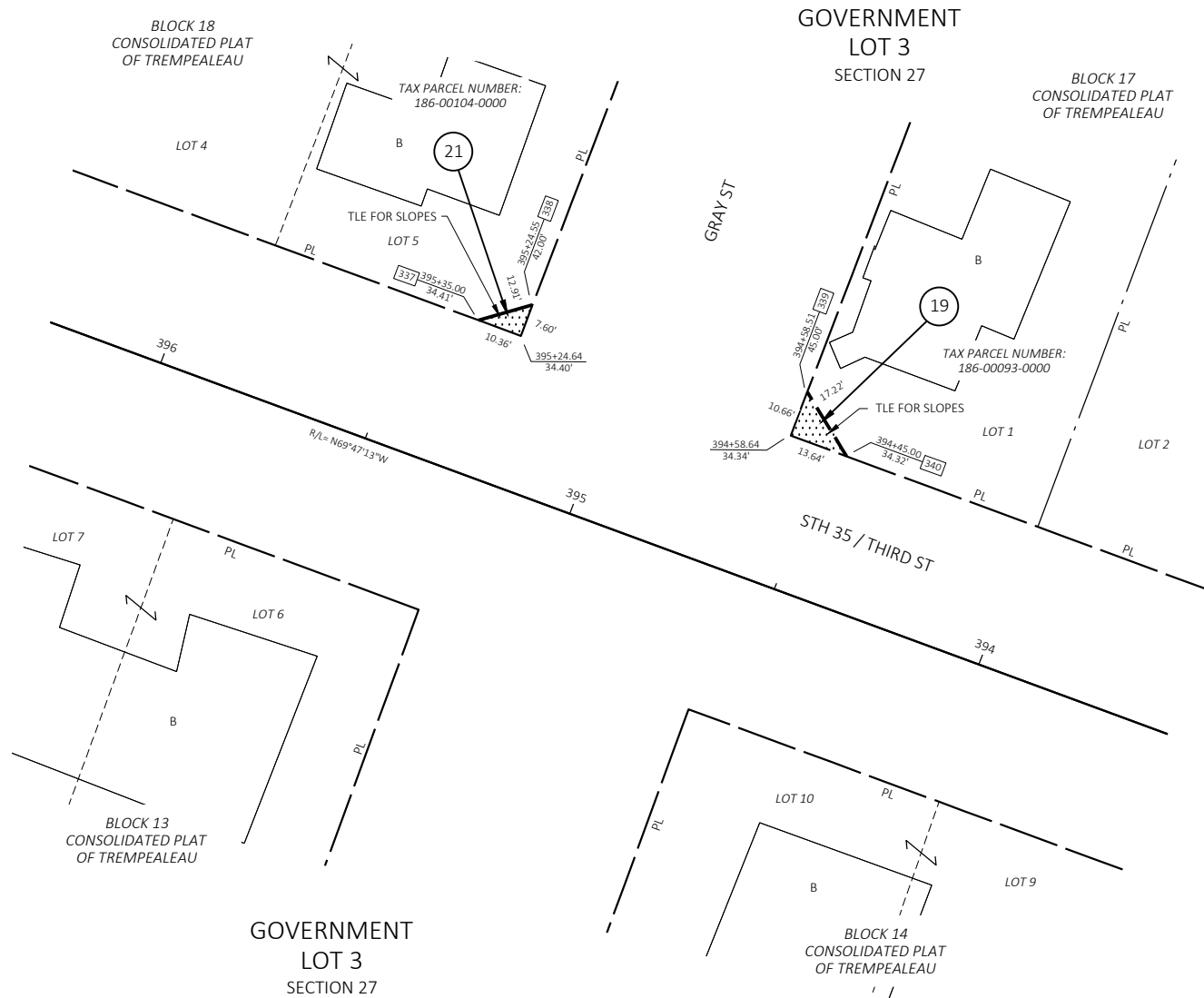
NOTES:  
THIS EXHIBIT IS A GRAPHIC REPRESENTATION AND IS FOR REFERENCE PURPOSES ONLY. REFER TO THE CONVEYANCE DOCUMENT FOR PARCEL RELATED DETAILS.

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R/W PROJECT NUMBER: 7140-00-20 SHEET NUMBER: 9

TLE ACQUISITION EXHIBIT  
LA CROSSE - TREMPPEALEAU  
LA CROSSE / TREMPPEALEAU COUNTY LINE TO 10TH ST  
TREMPEALEAU COUNTY  
THAT PART OF LOT 1, BLOCK 17 AND PART OF LOT 5, BLOCK 18 OF THE CONSOLIDATED PLAT OF TREMPPEALEAU, GOVERNMENT LOT 3, SECTION 27, T18N, R9W, VILLAGE OF TREMPPEALEAU, TREMPPEALEAU COUNTY, WISCONSIN.



**SCHEDULE OF LANDS & INTERESTS REQUIRED**

OWNERS NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE DEPARTMENT

PARCEL NUMBER	OWNER(S)	INTEREST REQUIRED	TLE SQUARE FEET
19	JOHN L AND KARRIE A ROBINSON	TLE	73
21	GABRIEL J HULL AND NICOLET A ROBINSON	TLE	39

**UTILITY INTERESTS REQUIRED**

UTILITY NUMBER	UTILITY OWNER(S)	INTEREST REQUIRED
N/A	N/A	N/A

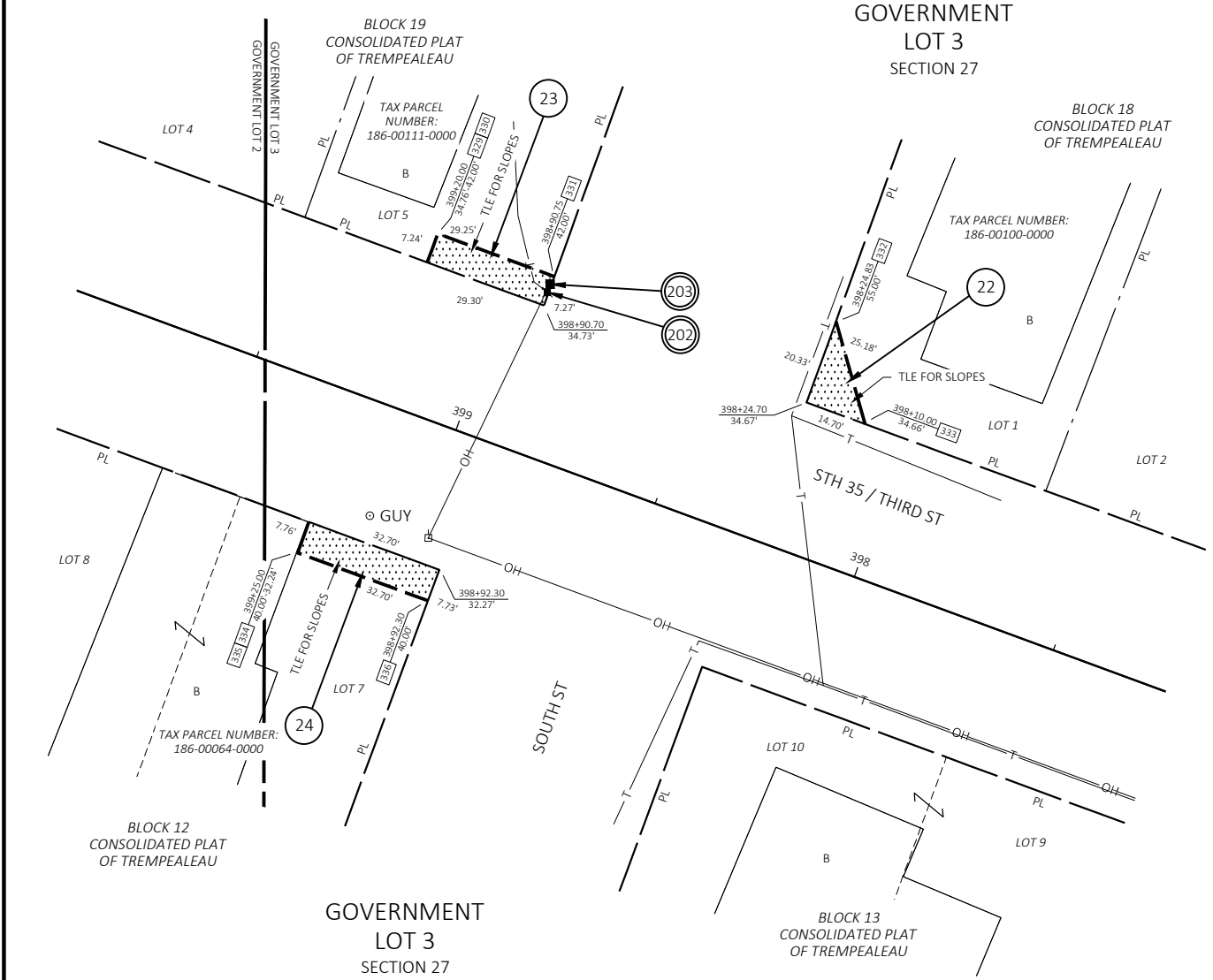
NOTES:  
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R/W PROJECT NUMBER: 7140-00-20 SHEET NUMBER: 10

TLE ACQUISITION EXHIBIT  
LA CROSSE - TREMPPEALEAU  
LA CROSSE / TREMPPEALEAU COUNTY LINE TO 10TH ST  
TREMPEALEAU COUNTY  
THAT PART OF LOT 1, BLOCK 18, PART OF LOT 5, BLOCK 19, AND PART OF LOT 7, BLOCK 12 OF THE CONSOLIDATED PLAT OF TREMPPEALEAU, GOVERNMENT LOT 3, SECTION 27, T18N, R9W, VILLAGE OF TREMPPEALEAU, TREMPPEALEAU COUNTY, WISCONSIN.



**SCHEDULE OF LANDS & INTERESTS REQUIRED**

OWNERS NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE DEPARTMENT

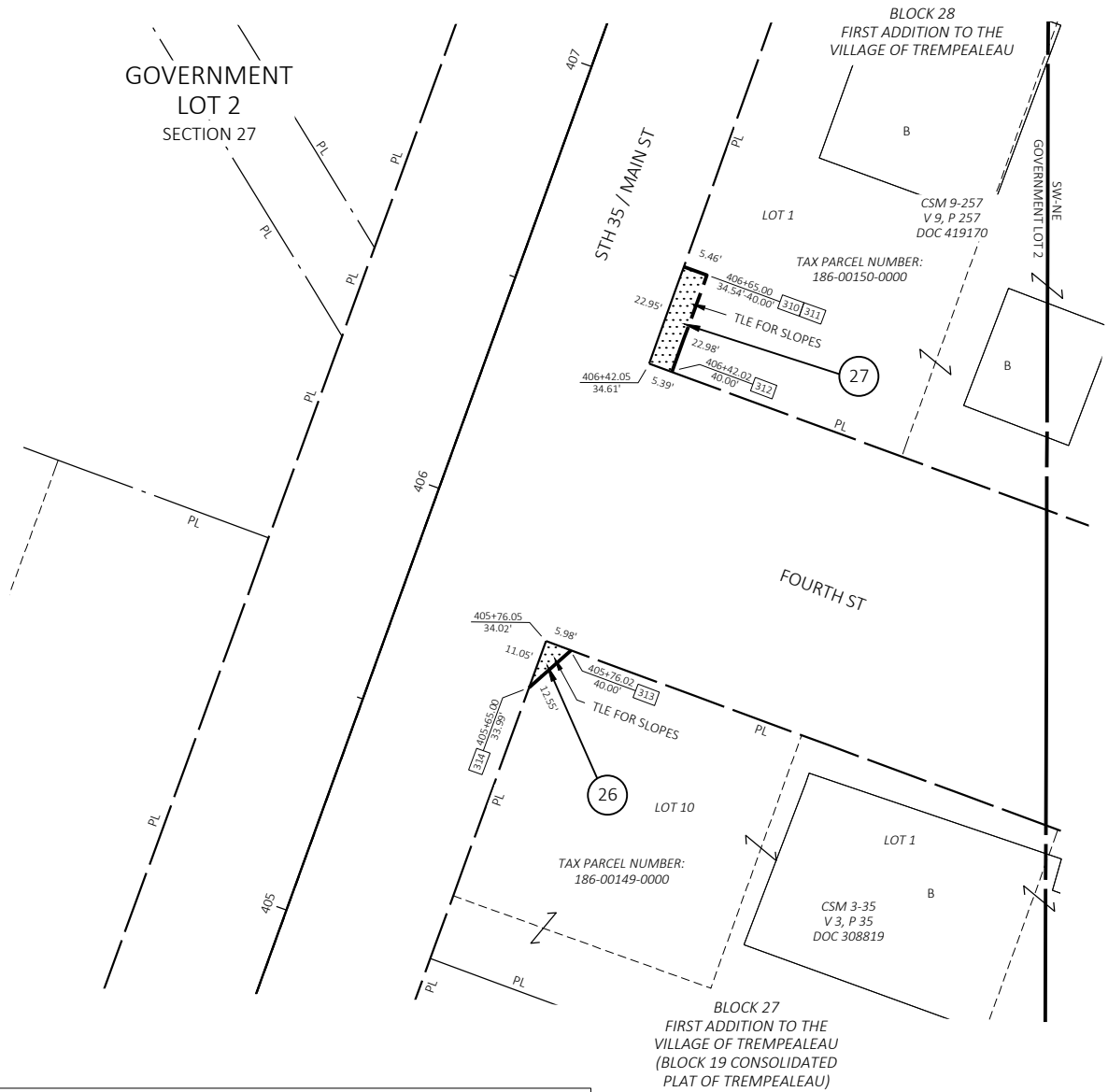
PARCEL NUMBER	OWNER(S)	INTEREST REQUIRED	TLE SQUARE FEET
22	ANDREW J WOLFE	TLE	149
23	WILLIAM B AND LORI ANN SCHAFFNER	TLE	212
24	VETERANS OF FOREIGN WARS POST NO 1915	TLE	253

**UTILITY INTERESTS REQUIRED**

UTILITY NUMBER	UTILITY OWNER(S)	INTEREST REQUIRED
203	TREMPEALEAU MUNICIPAL - ELECTRIC	NO RECORD OF EASEMENT
202	TRI-COUNTY COMMUNICATIONS COOPERATIVE	NO RECORD OF EASEMENT

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R/W PROJECT NUMBER: 7140-00-20 SHEET NUMBER: 11  
 TLE ACQUISITION EXHIBIT  
 LA CROSSE - TREMPPEALEAU  
 LA CROSSE / TREMPPEALEAU COUNTY LINE TO 10TH ST  
 TREMPPEALEAU COUNTY  
 STH 35  
 THAT PART OF LOT 10, BLOCK 27 AND PART OF LOT 1, CSM 9-257, V 9, P 257, DOC 419170, BEING PART OF BLOCK 28 OF THE CONSOLIDATED PLAT OF TREMPPEALEAU, GOVERNMENT LOT 2, SECTION 27, T18N, R9W, VILLAGE OF TREMPPEALEAU, TREMPPEALEAU COUNTY, WISCONSIN.



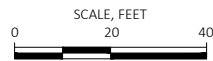
**SCHEDULE OF LANDS & INTERESTS REQUIRED**

OWNERS NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE DEPARTMENT

PARCEL NUMBER	OWNER(S)	INTEREST REQUIRED	TLE SQUARE FEET
26	JOHN MICHAEL LORD	TLE	33
27	PHILLIP C BUNTON AND BARBARA A EVERSON-BUNTON	TLE	125

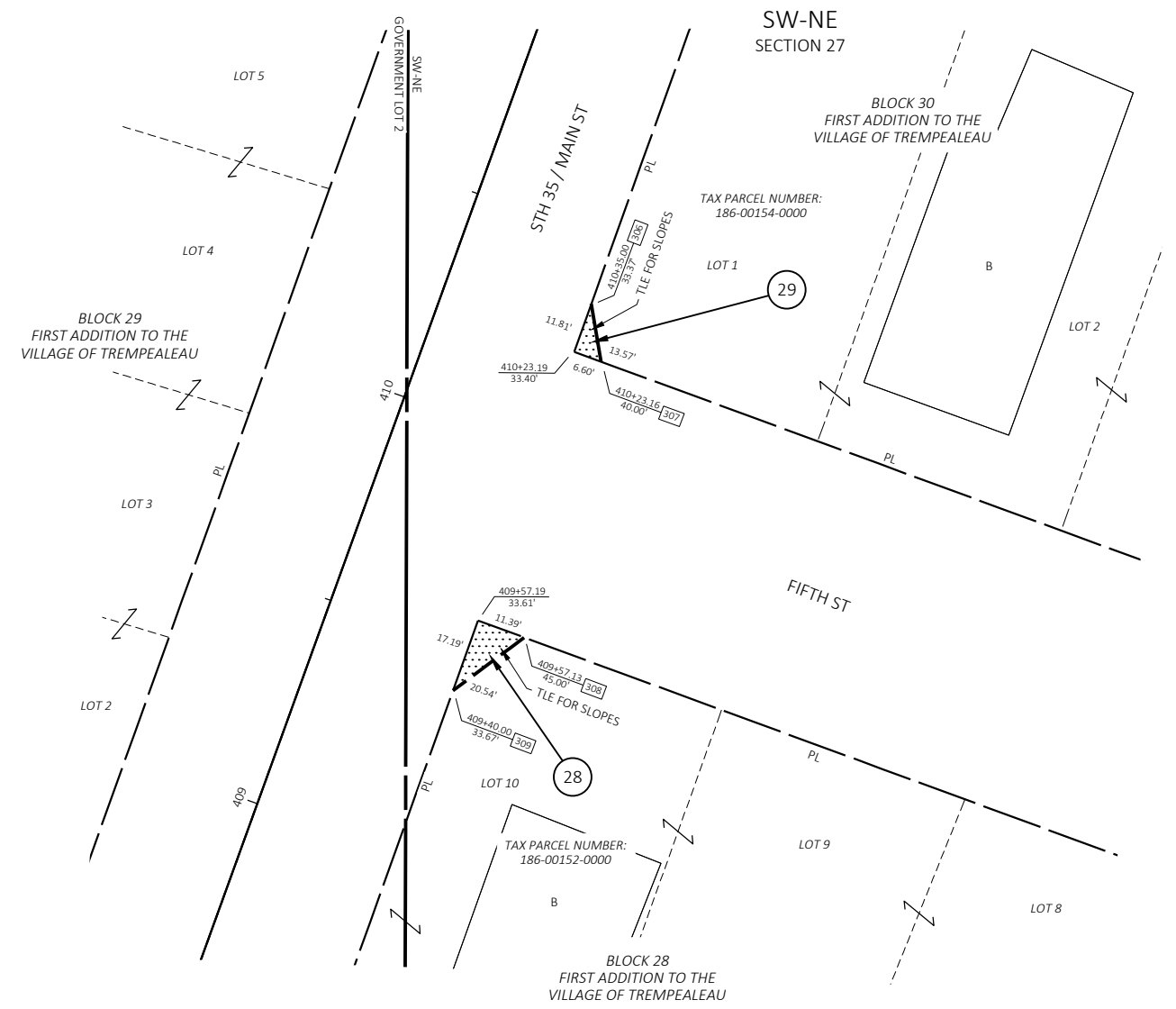
**UTILITY INTERESTS REQUIRED**

UTILITY NUMBER	UTILITY OWNER(S)	INTEREST REQUIRED
N/A	N/A	N/A



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R/W PROJECT NUMBER: 7140-00-20 SHEET NUMBER: 12  
 TLE ACQUISITION EXHIBIT  
 LA CROSSE - TREMPPEALEAU  
 LA CROSSE / TREMPPEALEAU COUNTY LINE TO 10TH ST  
 TREMPPEALEAU COUNTY  
 STH 35  
 THAT PART OF LOT 10, BLOCK 28, AND PART OF LOT 1, BLOCK 30 OF THE FIRST ADDITION TO THE VILLAGE OF TREMPPEALEAU, SW 1/4 - NE 1/4, SECTION 27, T18N, R9W, VILLAGE OF TREMPPEALEAU, TREMPPEALEAU COUNTY, WISCONSIN.



**SCHEDULE OF LANDS & INTERESTS REQUIRED**

OWNERS NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE DEPARTMENT

PARCEL NUMBER	OWNER(S)	INTEREST REQUIRED	TLE SQUARE FEET
28	ETHEL L OUELLETTE	TLE	98
29	DAVID T DUTTON	TLE	39

**UTILITY INTERESTS REQUIRED**

UTILITY NUMBER	UTILITY OWNER(S)	INTEREST REQUIRED
N/A	N/A	N/A



NOTES:  
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R/W PROJECT NUMBER: 7140-00-20 SHEET NUMBER: 13  
 TLE ACQUISITION EXHIBIT  
 LA CROSSE - TREMPPEALEAU  
 LA CROSSE / TREMPPEALEAU COUNTY LINE TO 10TH ST  
 TREMPPEALEAU COUNTY  
 STH 35  
 THAT PART OF LOT 10, BLOCK 30 AND PART OF LOT 1, BLOCK 31 OF THE FIRST ADDITION TO THE VILLAGE OF TREMPPEALEAU, SW 1/4 - NE 1/4, SECTION 27, T18N, R9W, VILLAGE OF TREMPPEALEAU, TREMPPEALEAU COUNTY, WISCONSIN.



**SCHEDULE OF LANDS & INTERESTS REQUIRED**

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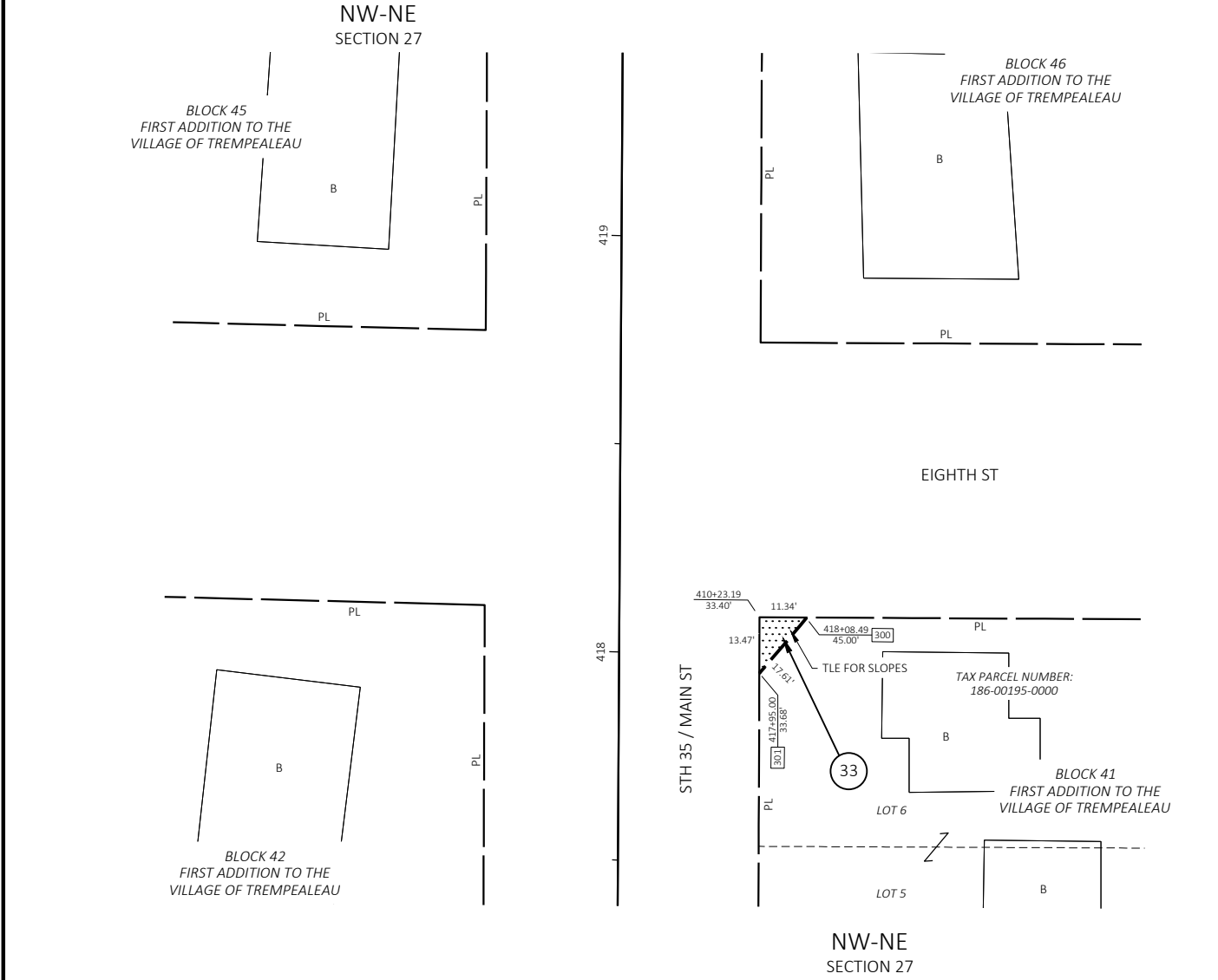
PARCEL NUMBER	OWNER(S)	INTEREST REQUIRED	TLE SQUARE FEET
31	AUDRA R AND TOBIAS D KARWOSKI	TLE	48
32	EVAN HAGEN AND AMANDA SWANSON	TLE	40

**UTILITY INTERESTS REQUIRED**

UTILITY NUMBER	UTILITY OWNER(S)	INTEREST REQUIRED
N/A	N/A	N/A

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R/W PROJECT NUMBER: 7140-00-20 SHEET NUMBER: 14  
 TLE ACQUISITION EXHIBIT  
 LA CROSSE - TREMPPEALEAU  
 LA CROSSE / TREMPPEALEAU COUNTY LINE TO 10TH ST  
 TREMPPEALEAU COUNTY  
 STH 35  
 THAT PART OF LOT 6, BLOCK 41 OF THE FIRST ADDITION TO THE VILLAGE OF TREMPPEALEAU, NW 1/4 - NE 1/4, SECTION 27, T18N, R9W, VILLAGE OF TREMPPEALEAU, TREMPPEALEAU COUNTY, WISCONSIN.



**SCHEDULE OF LANDS & INTERESTS REQUIRED**

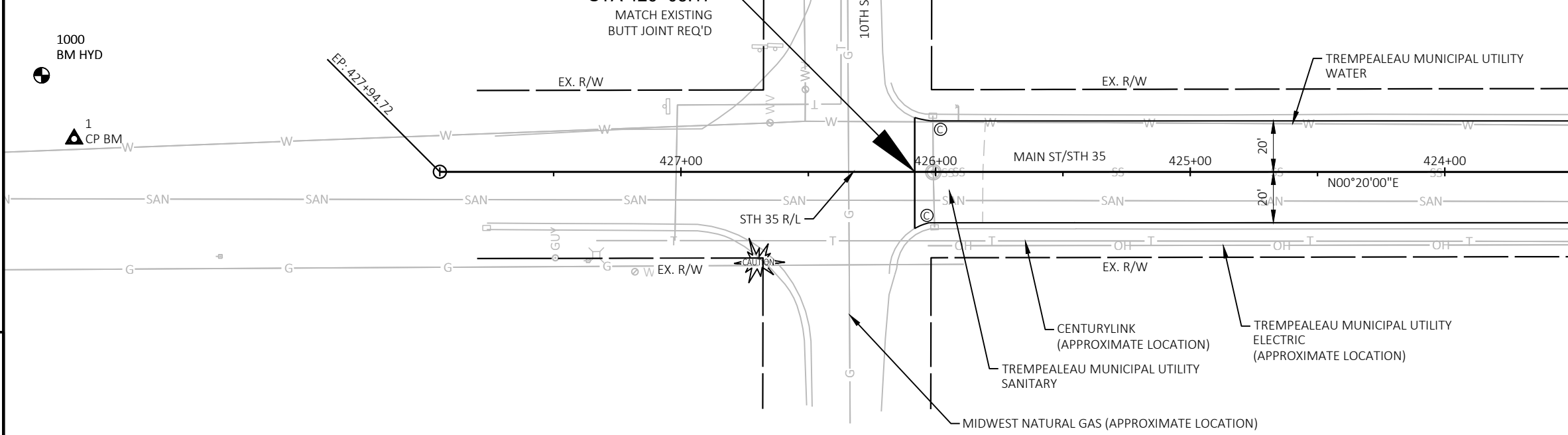
OWNERS NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE DEPARTMENT

PARCEL NUMBER	OWNER(S)	INTEREST REQUIRED	TLE SQUARE FEET
33	NICHOLAS A SCHOCK	TLE	76

**UTILITY INTERESTS REQUIRED**

UTILITY NUMBER	UTILITY OWNER(S)	INTEREST REQUIRED
N/A	N/A	N/A

BENCHMARK/CONTROL POINT TABLE						
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION	DESC
1	0+00.00	0.00	310551.132	823583.474	709.24	CP BM
1000	0+00.00	0.00	310563.796	823608.520	705.48	BM HYD



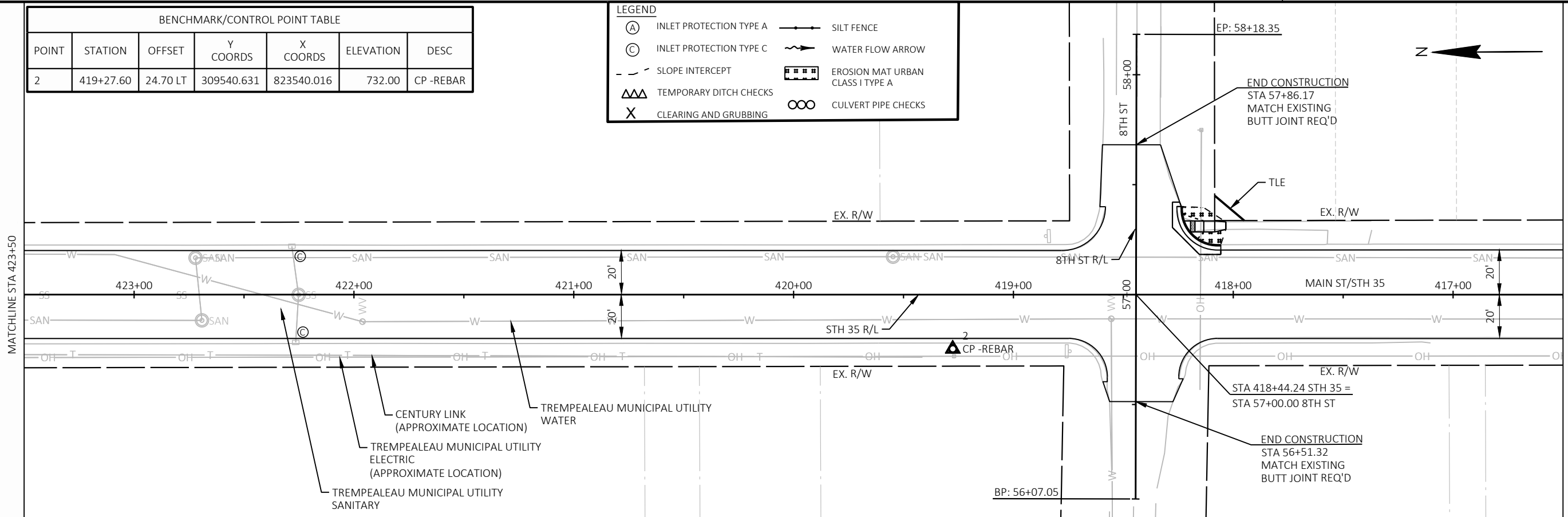
MATCHLINE STA 423+50

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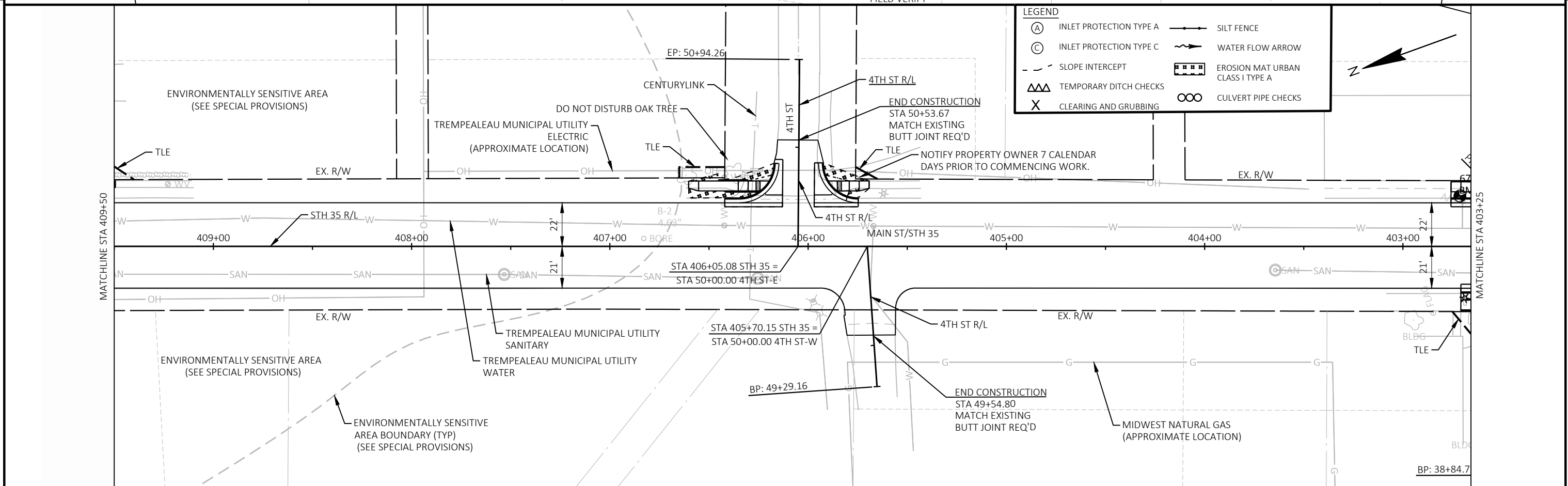
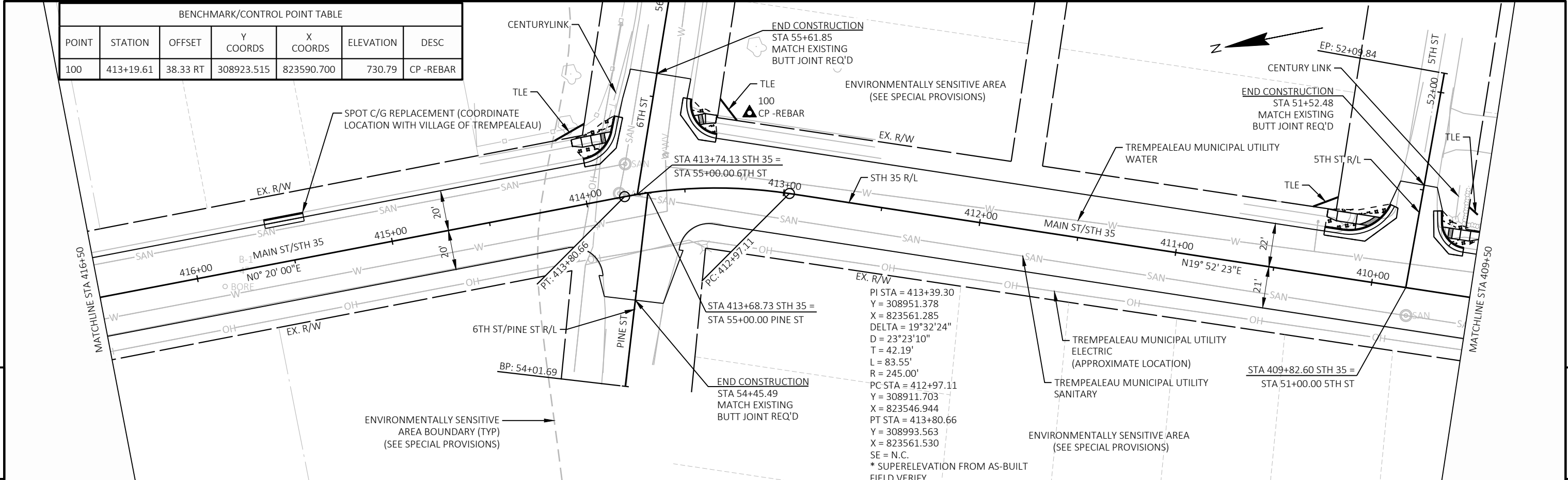
BENCHMARK/CONTROL POINT TABLE						
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION	DESC
2	419+27.60	24.70 LT	309540.631	823540.016	732.00	CP -REBAR

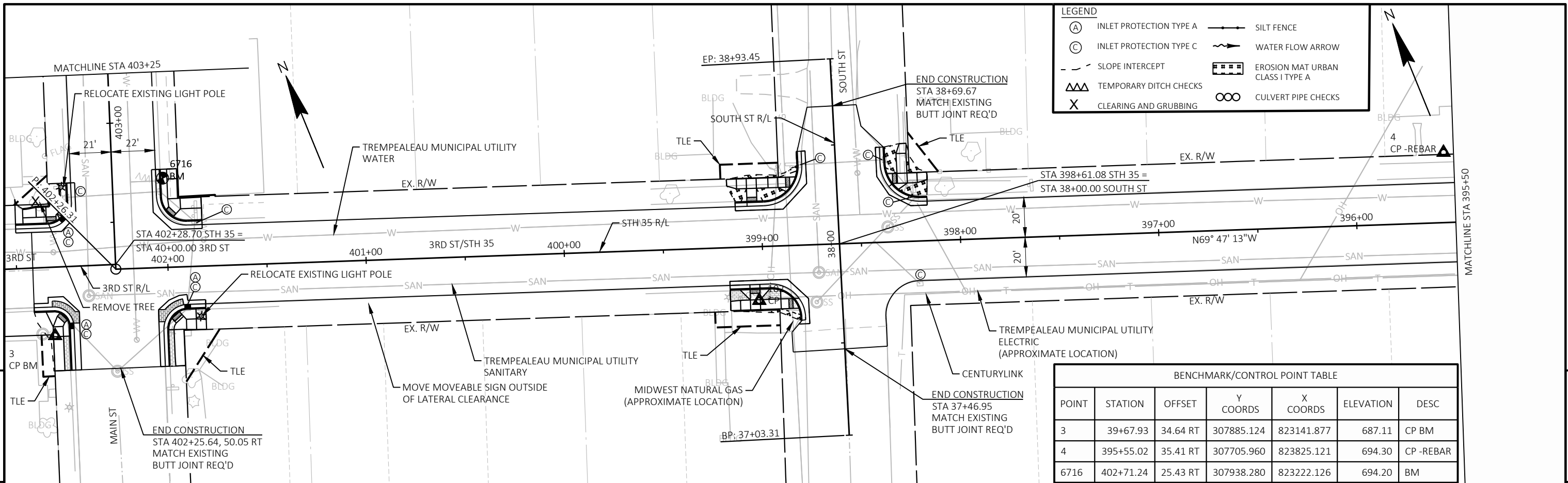
LEGEND	
(A)	INLET PROTECTION TYPE A
(C)	INLET PROTECTION TYPE C
- - -	SLOPE INTERCEPT
△△△	TEMPORARY DITCH CHECKS
X	CLEARING AND GRUBBING
→	SILT FENCE
→	WATER FLOW ARROW
	EROSION MAT URBAN CLASS I TYPE A
∞	CULVERT PIPE CHECKS



MATCHLINE STA 416+50

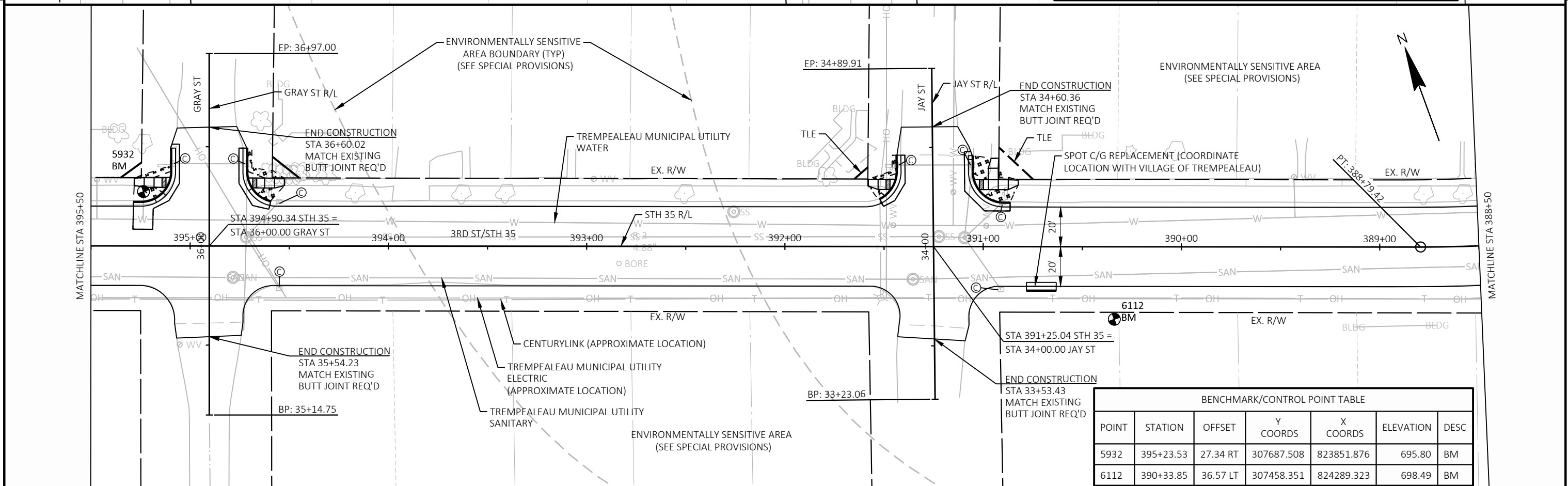
BENCHMARK/CONTROL POINT TABLE						
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION	DESC
100	413+19.61	38.33 RT	308923.515	823590.700	730.79	CP -REBAR





**BENCHMARK/CONTROL POINT TABLE**

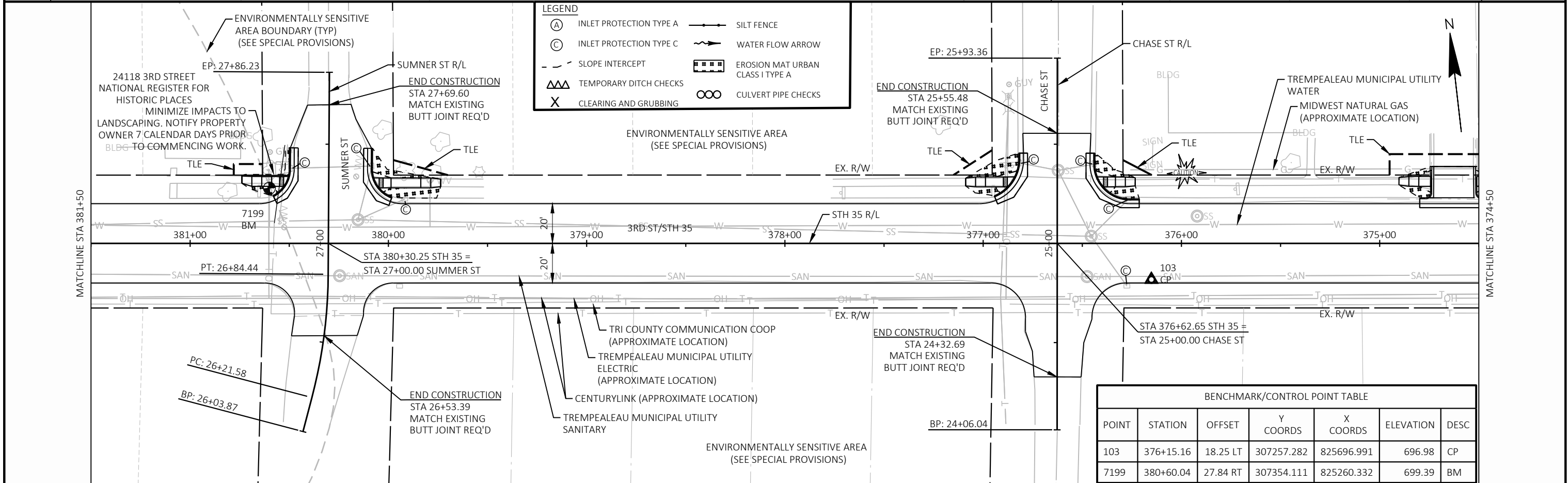
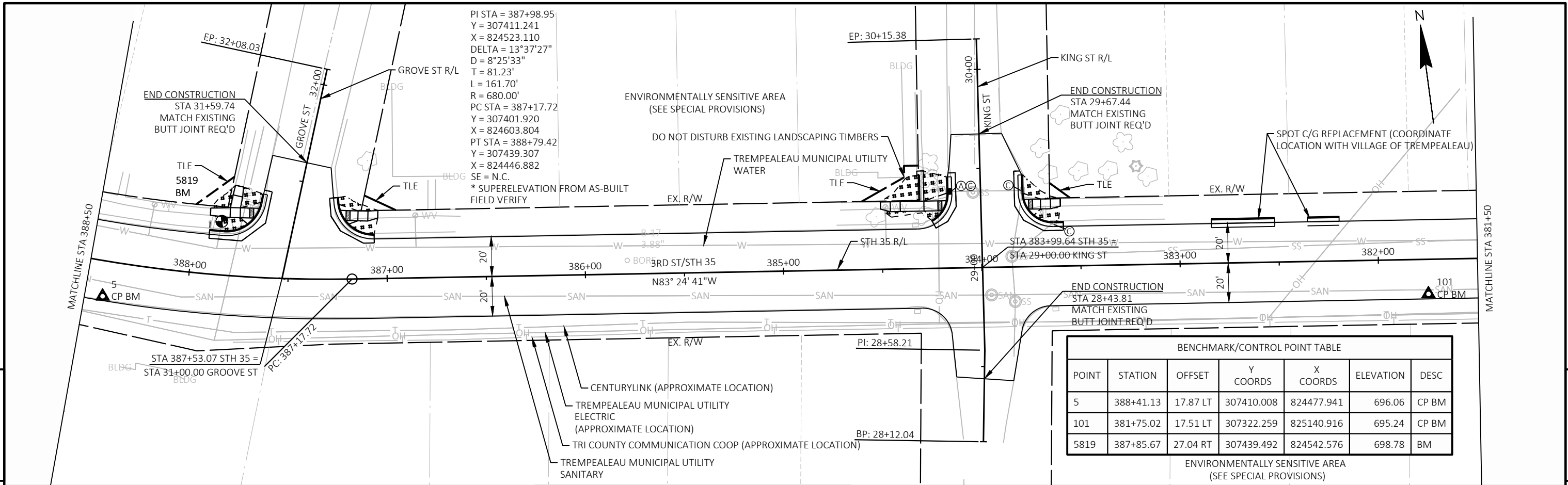
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION	DESC
3	39+67.93	34.64 RT	307885.124	823141.877	687.11	CP BM
4	395+55.02	35.41 RT	307705.960	823825.121	694.30	CP -REBAR
6716	402+71.24	25.43 RT	307938.280	823222.126	694.20	BM

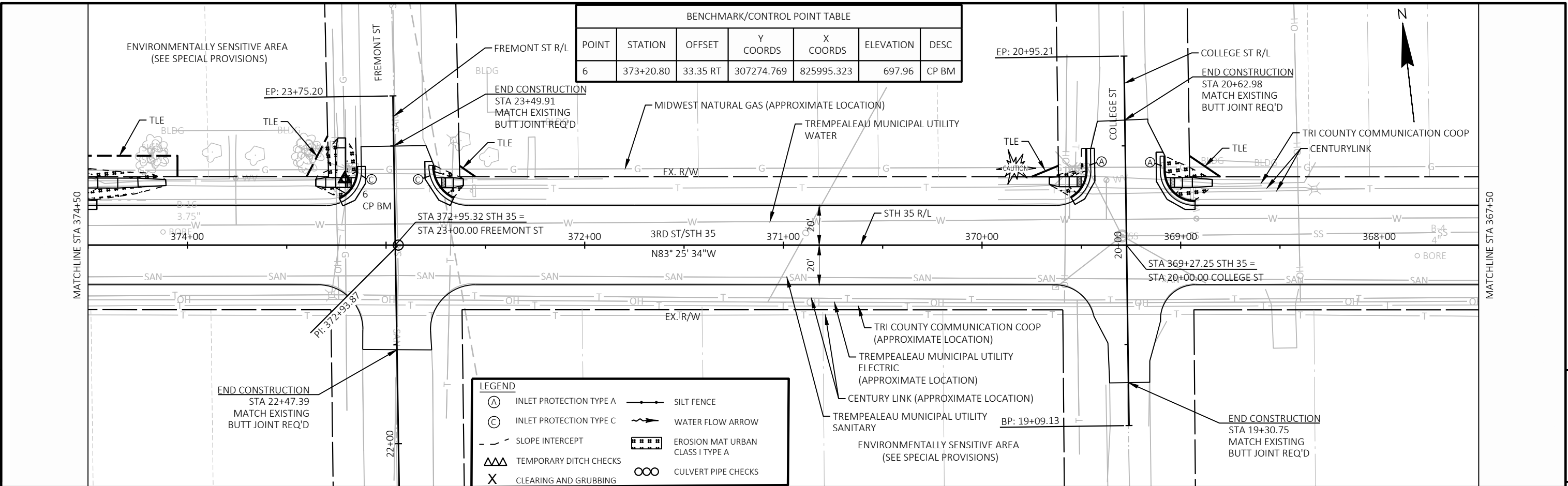


**BENCHMARK/CONTROL POINT TABLE**

POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION	DESC
5932	395+23.53	27.34 RT	307687.508	823851.876	695.80	BM
6112	390+33.85	36.57 LT	307458.351	824289.323	698.49	BM

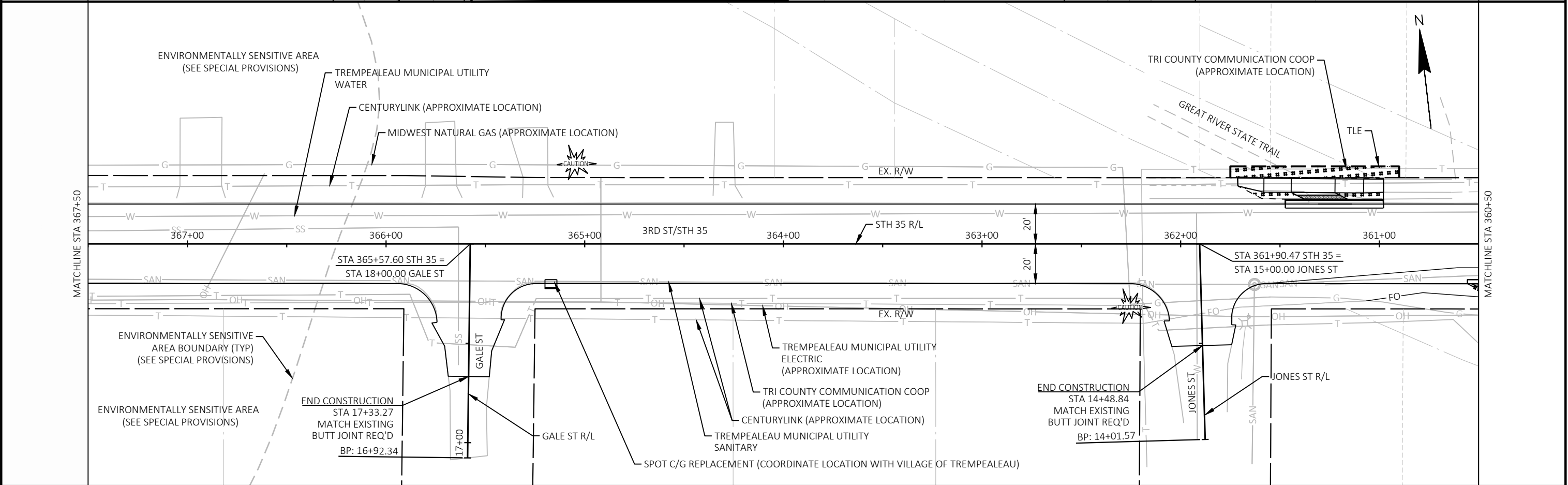




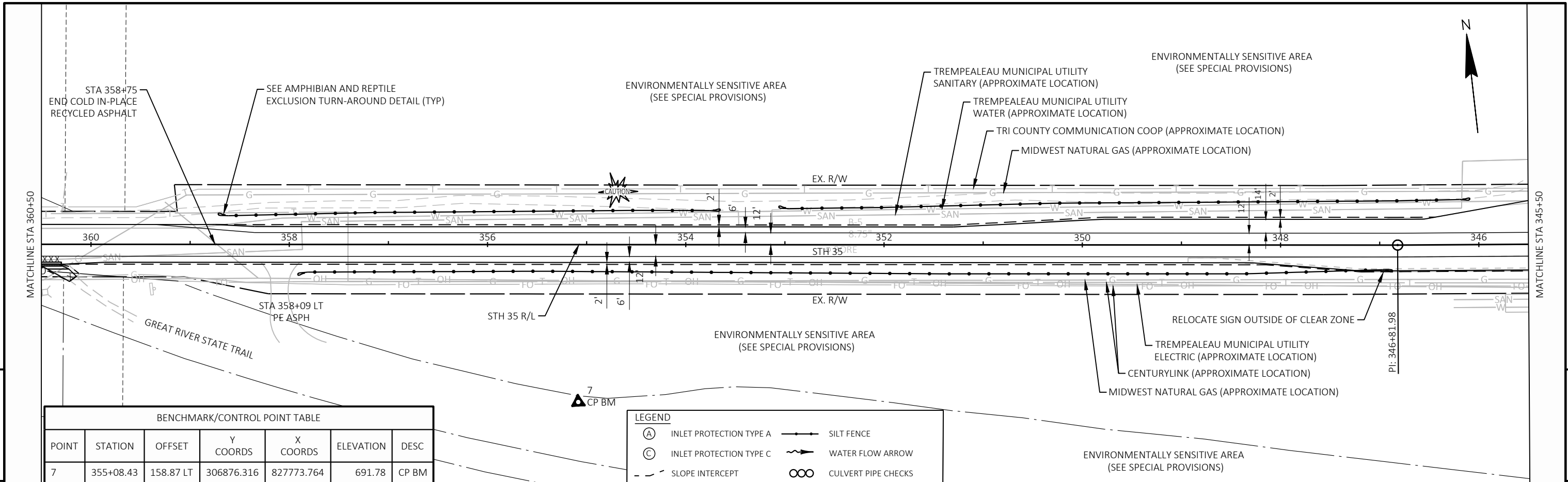


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PROJECT NO: 7140-00-70      HWY: STH 35      COUNTY: TREMPEALEAU      PLAN - STH 35      SHEET      E

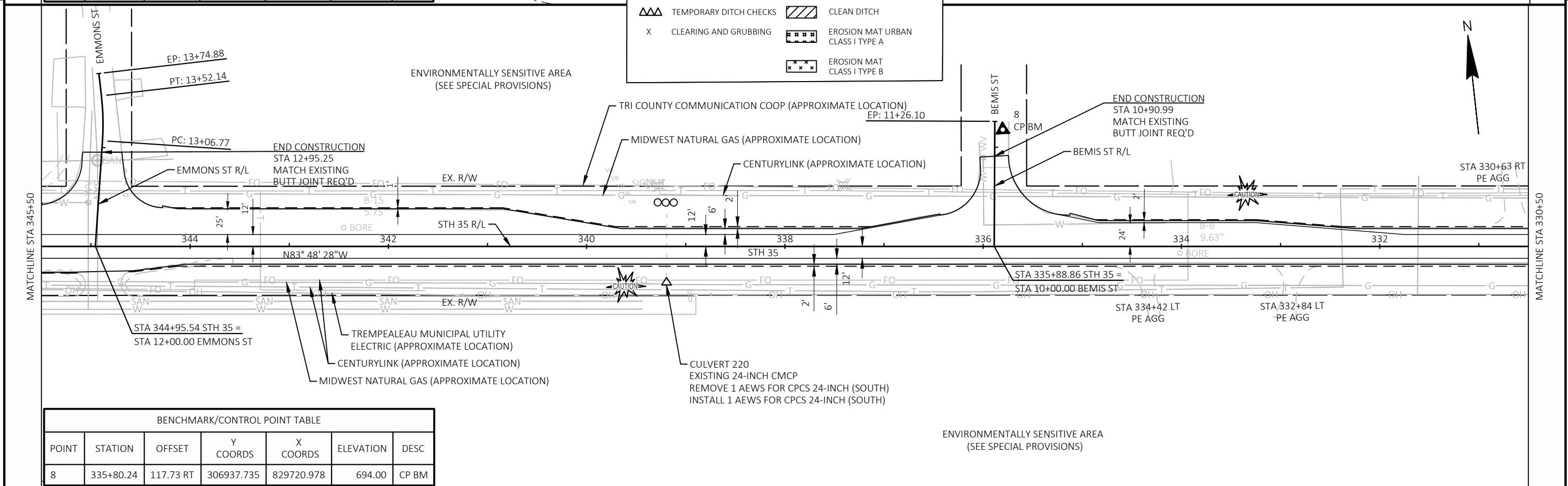


BENCHMARK/CONTROL POINT TABLE

POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION	DESC
7	355+08.43	158.87 LT	306876.316	827773.764	691.78	CP BM

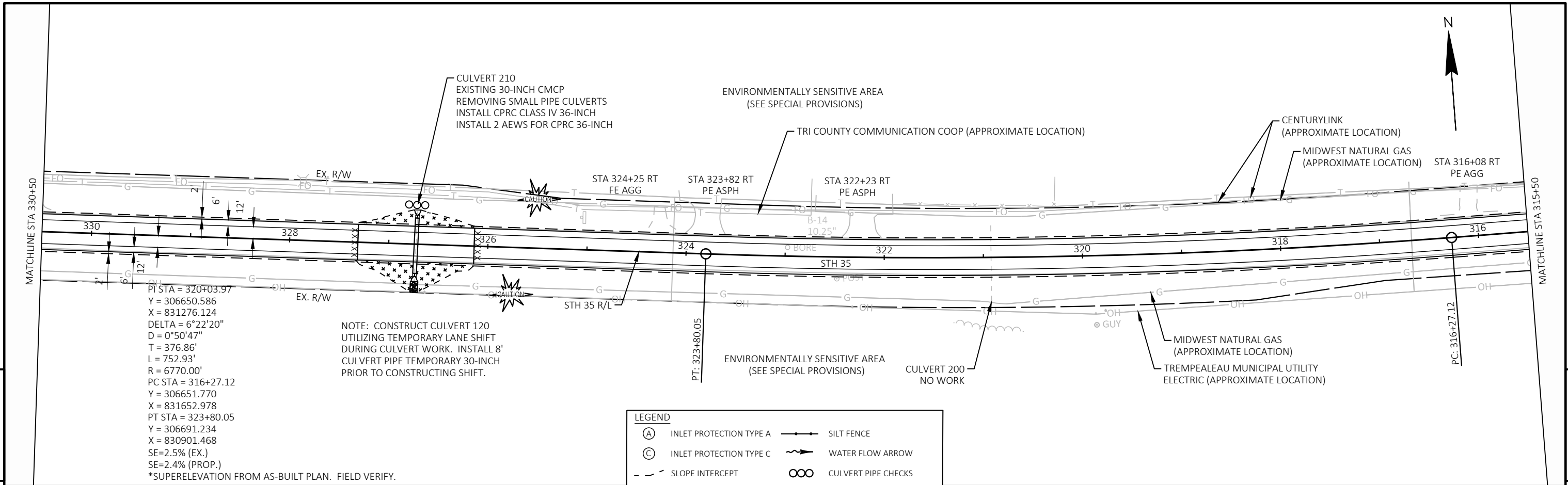
LEGEND

(A)	INLET PROTECTION TYPE A	— —	SILT FENCE
(C)	INLET PROTECTION TYPE C	~	WATER FLOW ARROW
-	SLOPE INTERCEPT	∞	CULVERT PIPE CHECKS
△△	TEMPORARY DITCH CHECKS	▨	CLEAN DITCH
X	CLEARING AND GRUBBING	■	EROSION MAT URBAN CLASS I TYPE A
		■	EROSION MAT CLASS I TYPE B



BENCHMARK/CONTROL POINT TABLE

POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION	DESC
8	335+80.24	117.73 RT	306937.735	829720.978	694.00	CP BM

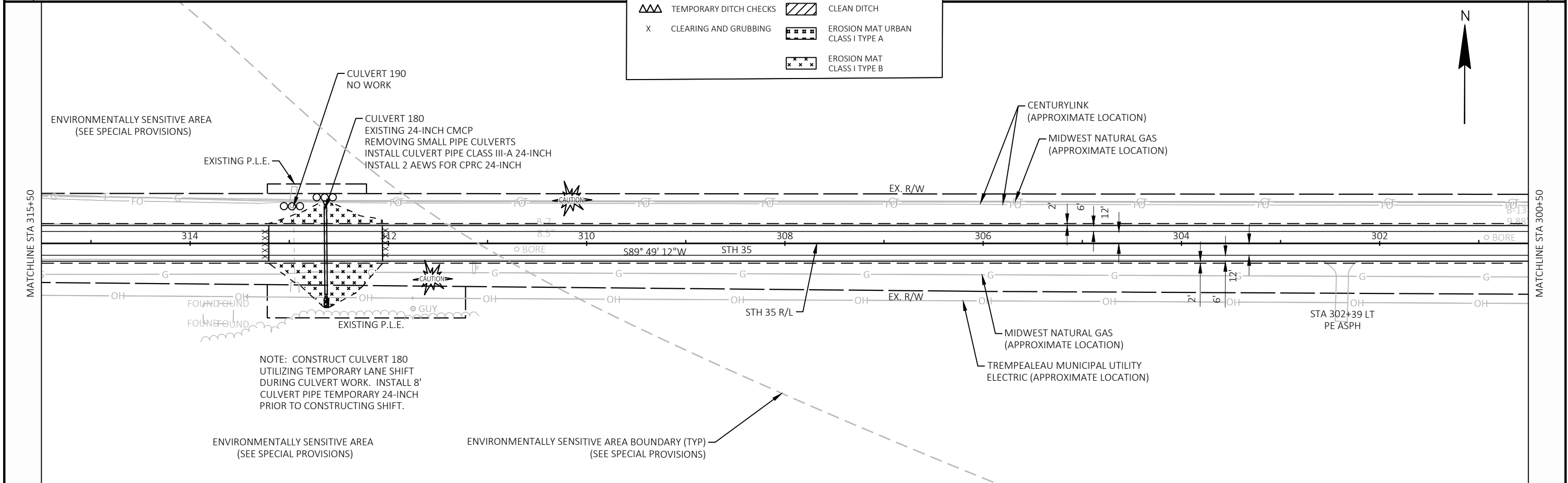


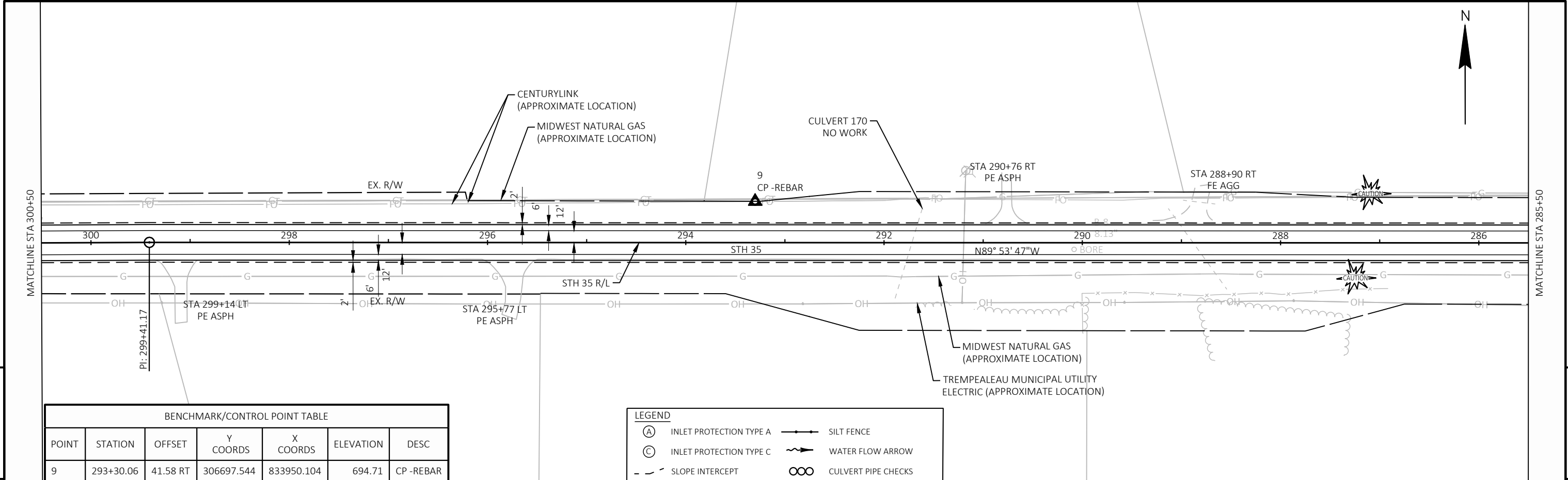
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**LEGEND**

(A)	INLET PROTECTION TYPE A	—	SILT FENCE
(C)	INLET PROTECTION TYPE C	→	WATER FLOW ARROW
- - -	SLOPE INTERCEPT	∞	CULVERT PIPE CHECKS
△△	TEMPORARY DITCH CHECKS	▨	CLEAN DITCH
X	CLEARING AND GRUBBING	■	EROSION MAT URBAN CLASS I TYPE A
		■	EROSION MAT CLASS I TYPE B





**BENCHMARK/CONTROL POINT TABLE**

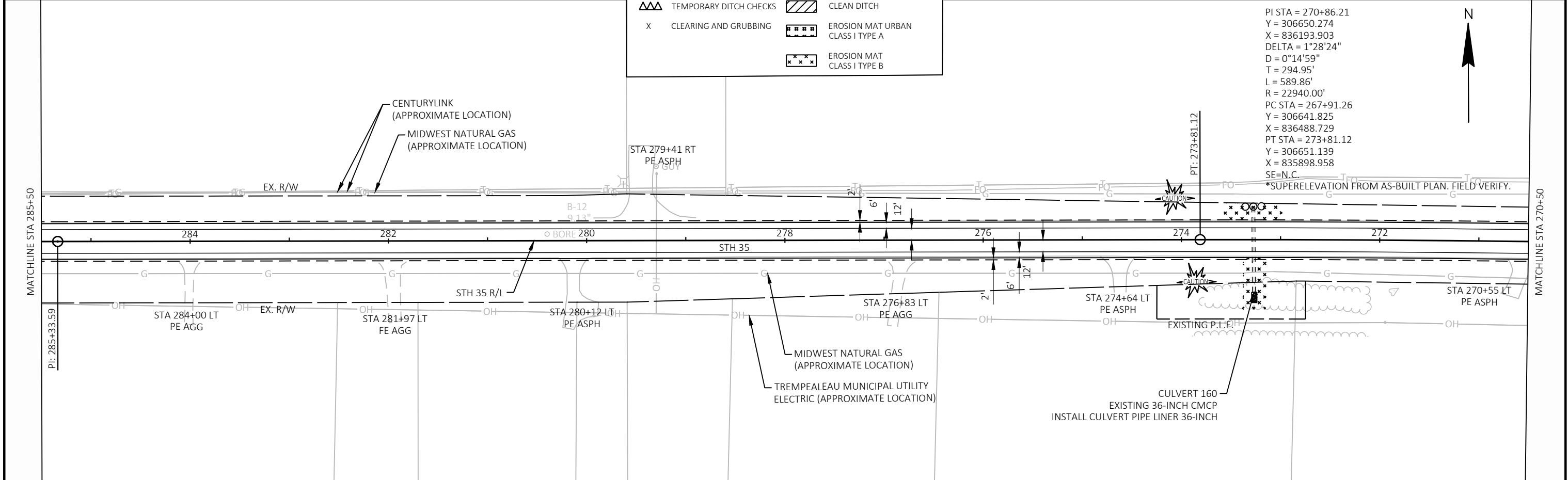
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION	DESC
9	293+30.06	41.58 RT	306697.544	833950.104	694.71	CP -REBAR

**LEGEND**

(A)	INLET PROTECTION TYPE A	— —	SILT FENCE
(C)	INLET PROTECTION TYPE C	~>	WATER FLOW ARROW
- - -	SLOPE INTERCEPT	∞	CULVERT PIPE CHECKS
△△	TEMPORARY DITCH CHECKS	▨	CLEAN DITCH
X	CLEARING AND GRUBBING	■	EROSION MAT URBAN CLASS I TYPE A
		■	EROSION MAT CLASS I TYPE B

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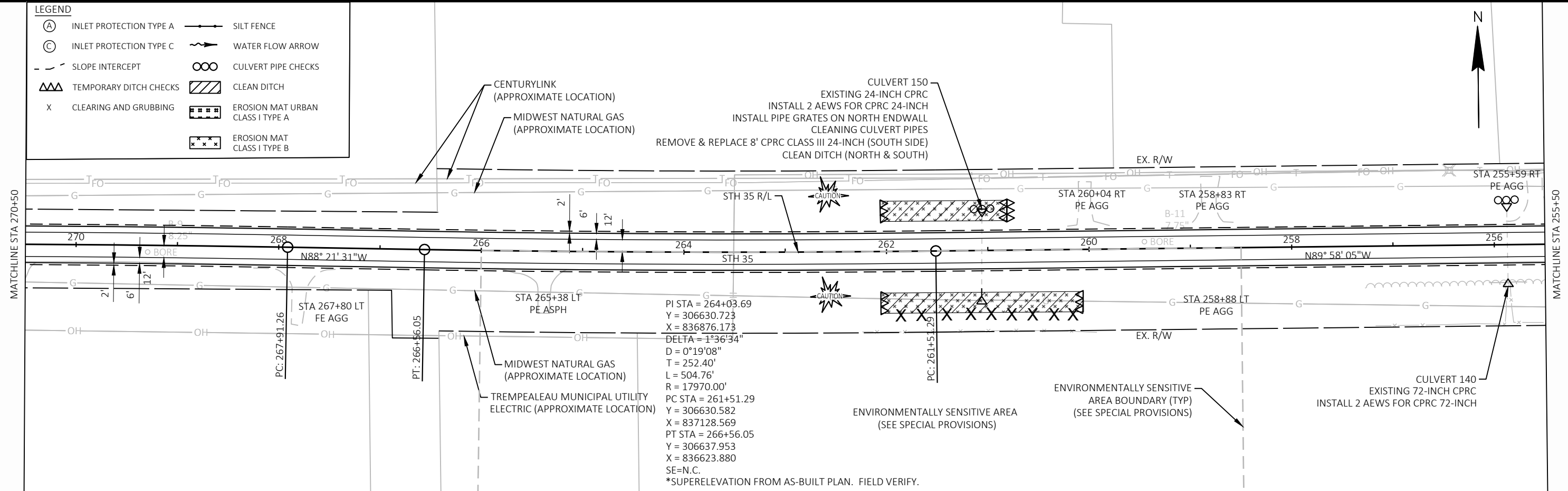
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PI STA = 270+86.21  
 Y = 306650.274  
 X = 836193.903  
 DELTA = 1°28'24"  
 D = 0°14'59"  
 T = 294.95'  
 L = 589.86'  
 R = 22940.00'  
 PC STA = 267+91.26  
 Y = 306641.825  
 X = 836488.729  
 PT STA = 273+81.12  
 Y = 306651.139  
 X = 835898.958  
 SE=N.C.  
 \*SUPERELEVATION FROM AS-BUILT PLAN. FIELD VERIFY.

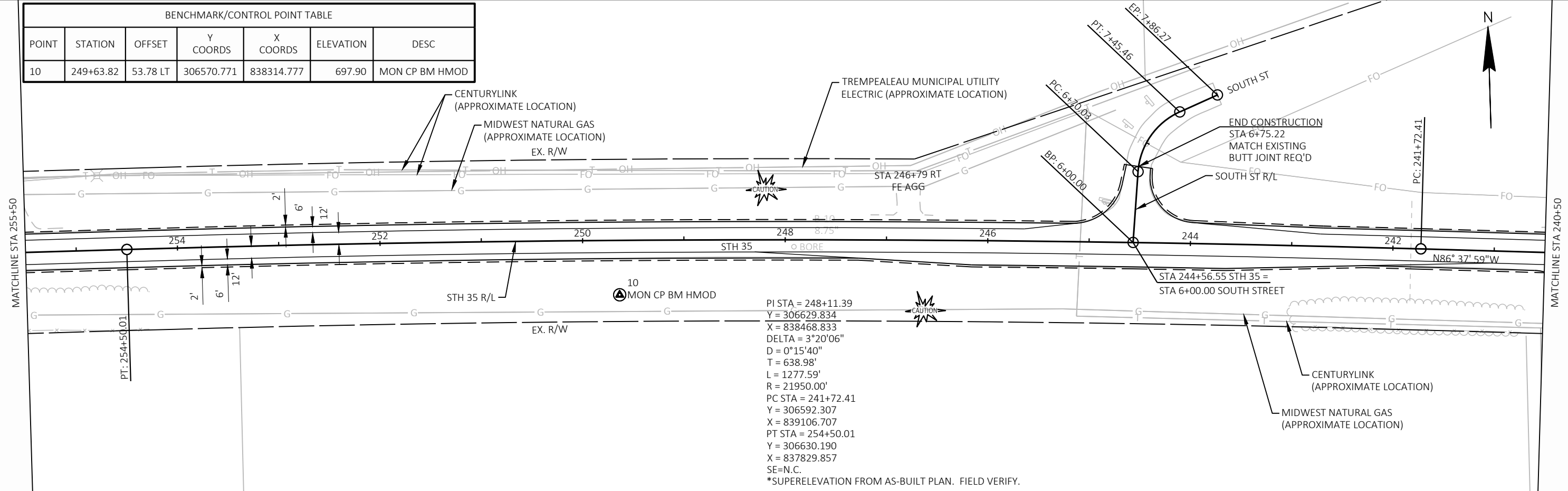
**LEGEND**

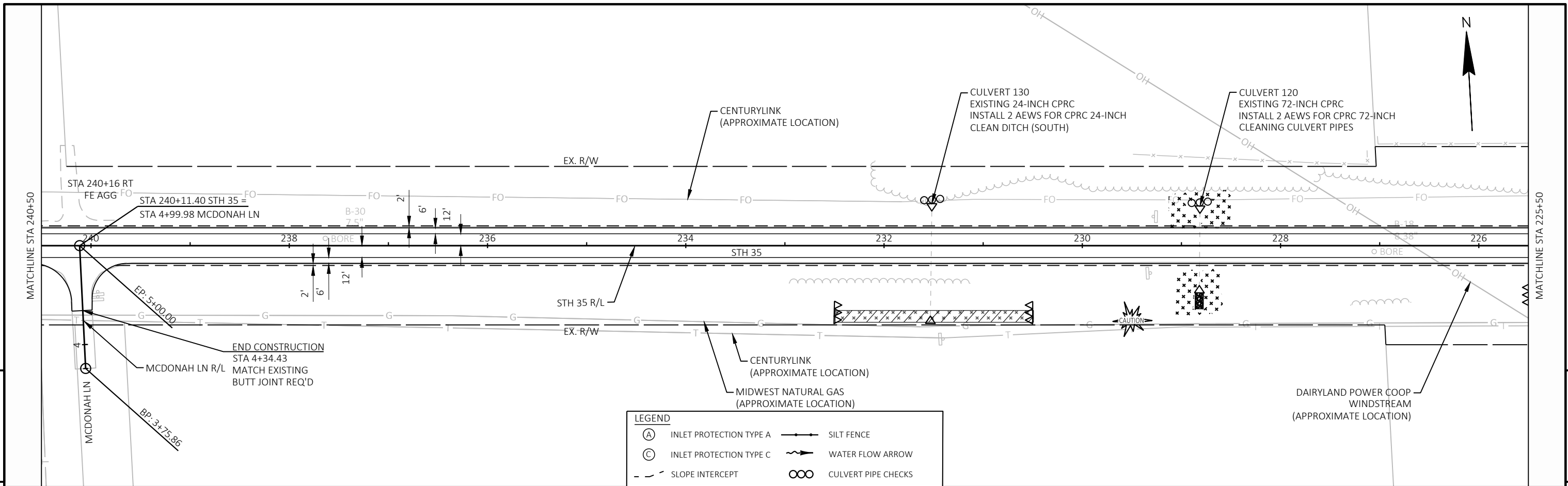
(A)	INLET PROTECTION TYPE A	— —	SILT FENCE
(C)	INLET PROTECTION TYPE C	~>	WATER FLOW ARROW
- - -	SLOPE INTERCEPT	∞	CULVERT PIPE CHECKS
△△△	TEMPORARY DITCH CHECKS	▨	CLEAN DITCH
X	CLEARING AND GRUBBING	■	EROSION MAT URBAN CLASS I TYPE A
		■	EROSION MAT CLASS I TYPE B



**BENCHMARK/CONTROL POINT TABLE**

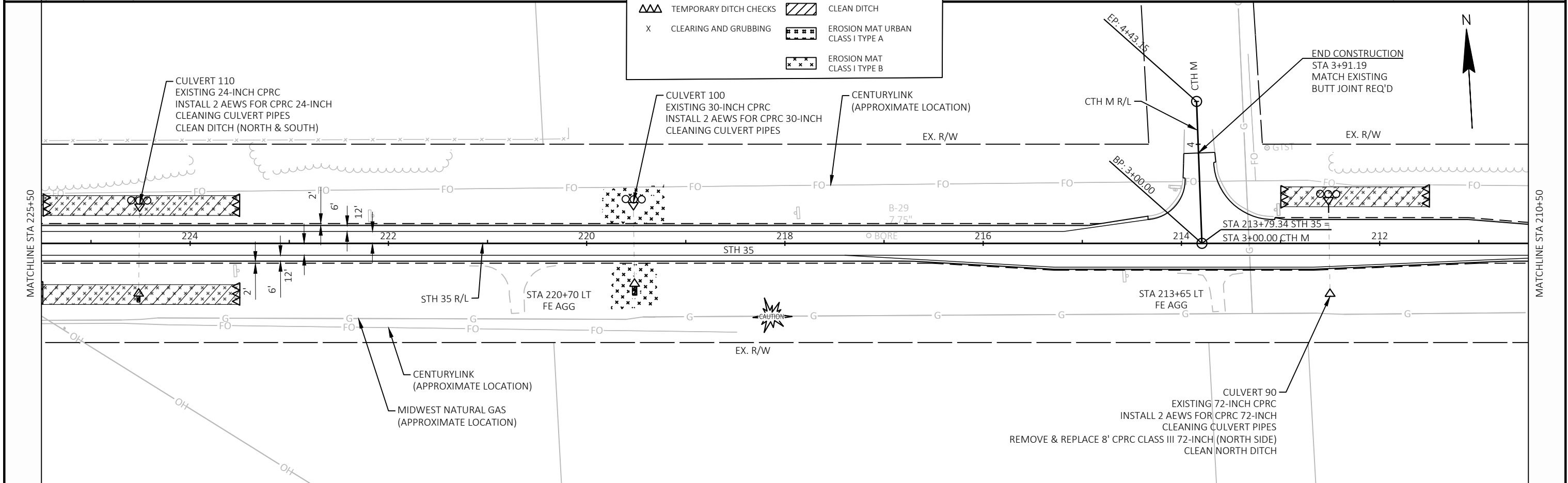
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION	DESC
10	249+63.82	53.78 LT	306570.771	838314.777	697.90	MON CP BM HMOD





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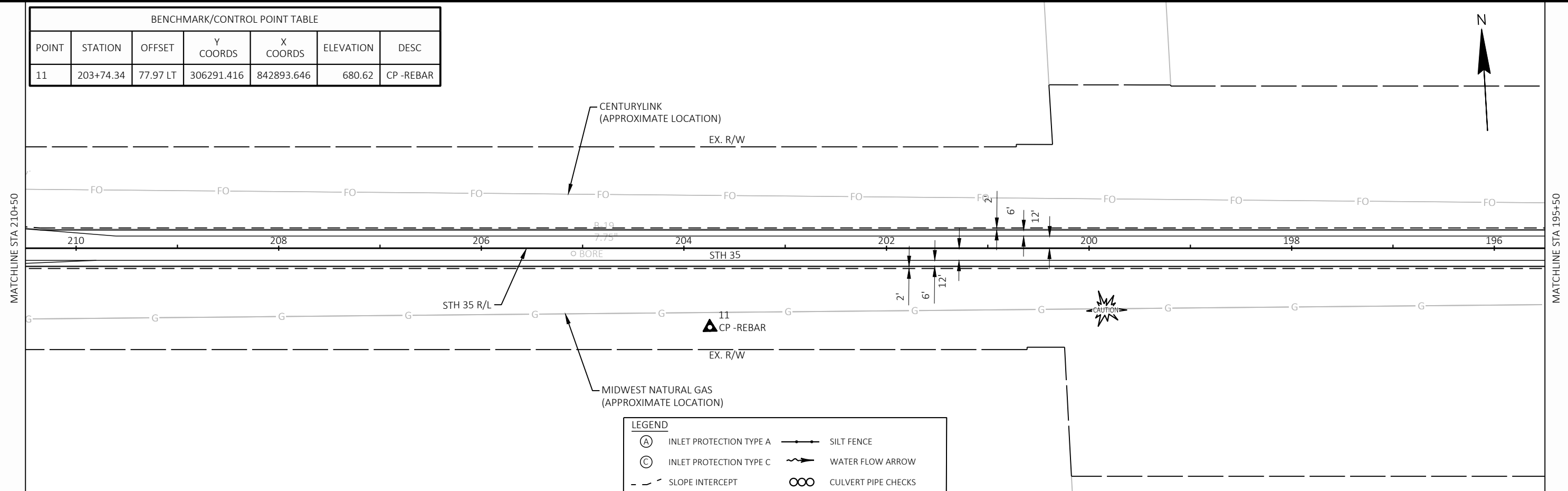
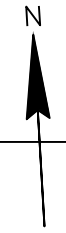


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PROJECT NO: 7140-00-70	HWY: STH 35	COUNTY: TREMPPEALEAU	PLAN - STH 35	SHEET	E
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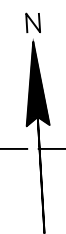
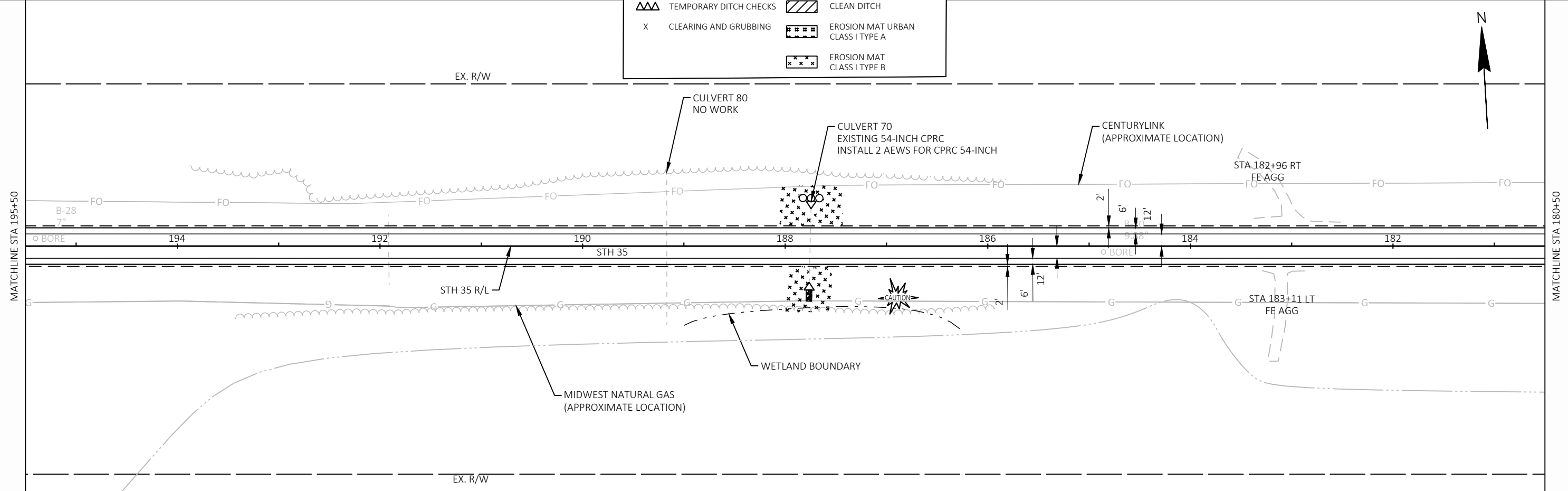
BENCHMARK/CONTROL POINT TABLE						
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION	DESC
11	203+74.34	77.97 LT	306291.416	842893.646	680.62	CP -REBAR



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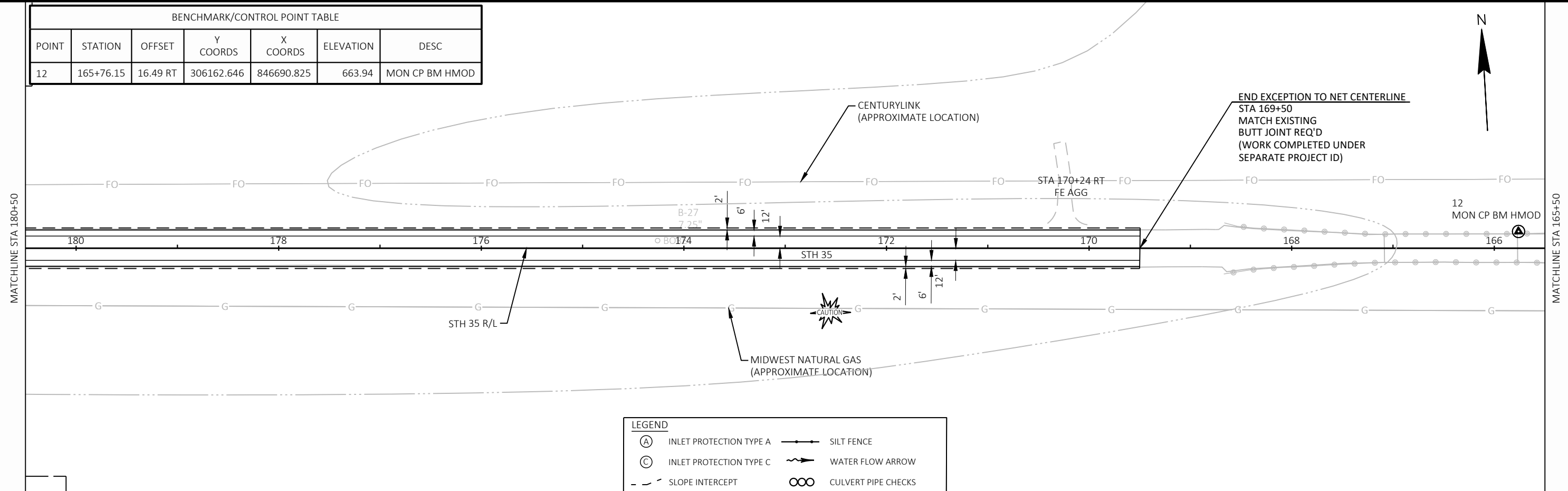
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LEGEND			
(A)	INLET PROTECTION TYPE A	— —	SILT FENCE
(C)	INLET PROTECTION TYPE C	~>	WATER FLOW ARROW
- - -	SLOPE INTERCEPT	∞	CULVERT PIPE CHECKS
△△	TEMPORARY DITCH CHECKS	▨	CLEAN DITCH
X	CLEARING AND GRUBBING	■	EROSION MAT URBAN CLASS I TYPE A
		■	EROSION MAT CLASS I TYPE B

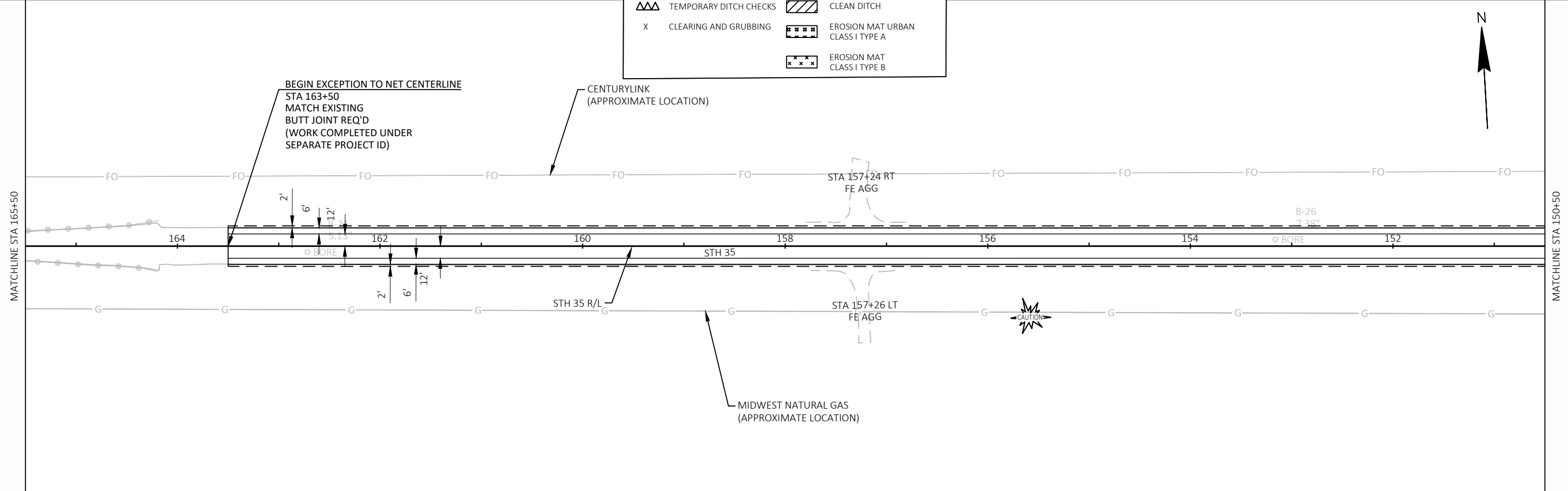




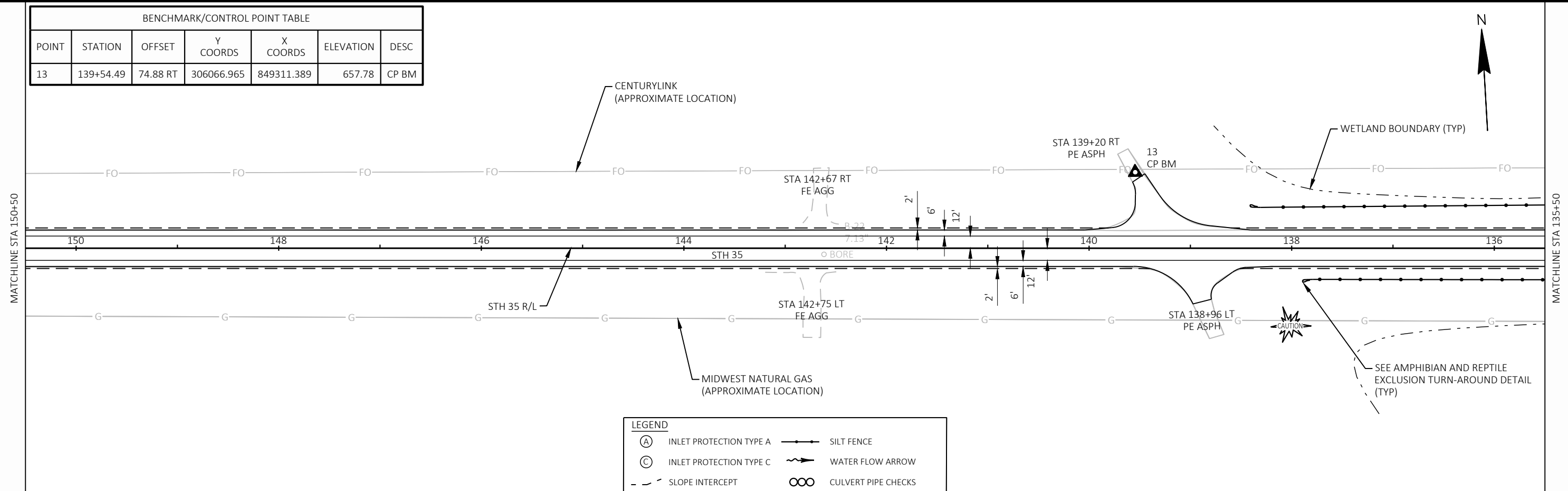
BENCHMARK/CONTROL POINT TABLE						
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION	DESC
12	165+76.15	16.49 RT	306162.646	846690.825	663.94	MON CP BM HMOD



LEGEND			
(A)	INLET PROTECTION TYPE A	— —	SILT FENCE
(C)	INLET PROTECTION TYPE C	~>	WATER FLOW ARROW
- - -	SLOPE INTERCEPT	∞	CULVERT PIPE CHECKS
△△	TEMPORARY DITCH CHECKS	▨	CLEAN DITCH
X	CLEARING AND GRUBBING	■	EROSION MAT URBAN CLASS I TYPE A
		■	EROSION MAT CLASS I TYPE B



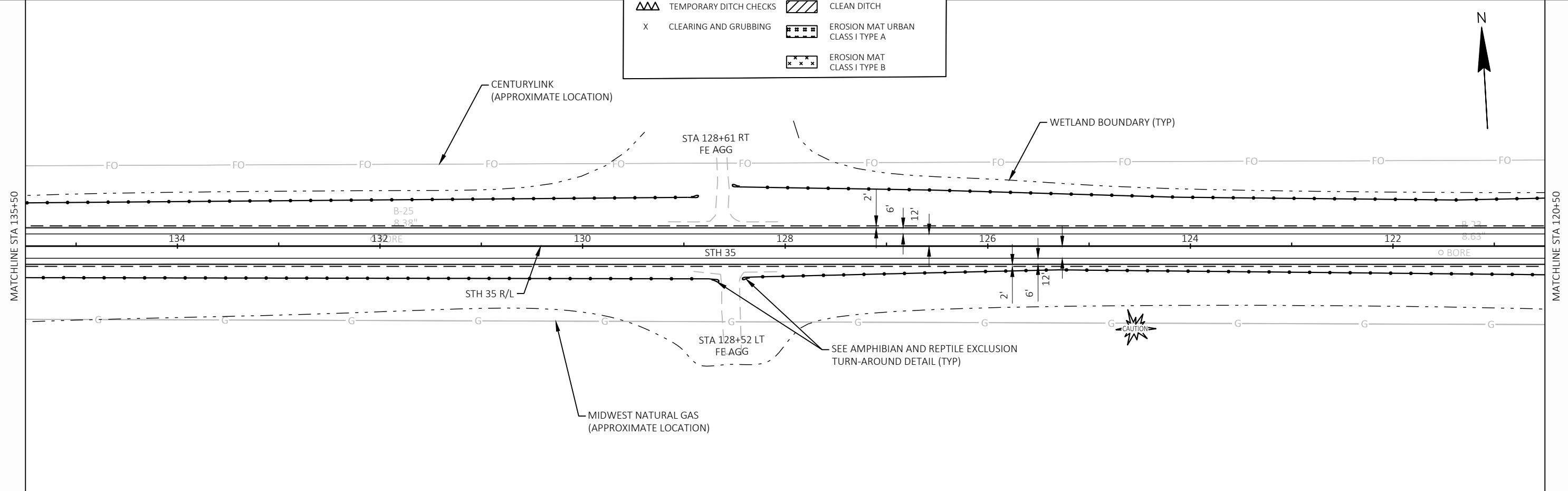
BENCHMARK/CONTROL POINT TABLE						
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION	DESC
13	139+54.49	74.88 RT	306066.965	849311.389	657.78	CP BM



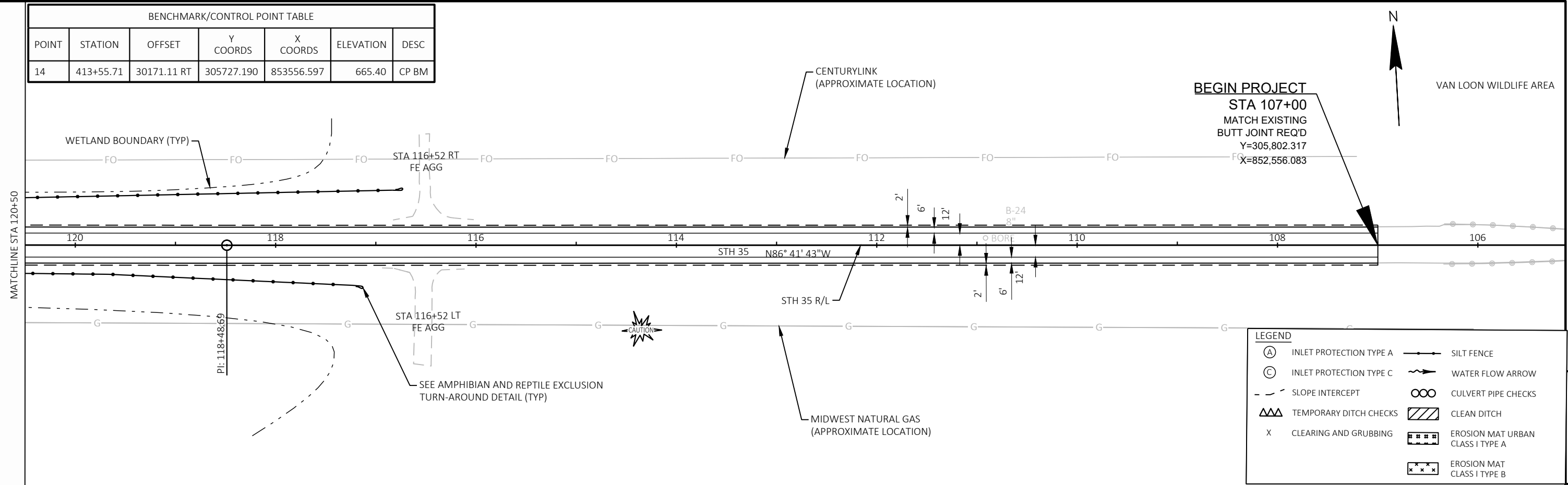
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LEGEND			
(A)	INLET PROTECTION TYPE A	—●—	SILT FENCE
(C)	INLET PROTECTION TYPE C	—▲—	WATER FLOW ARROW
- - -	SLOPE INTERCEPT	∞	CULVERT PIPE CHECKS
▲▲	TEMPORARY DITCH CHECKS	▨	CLEAN DITCH
X	CLEARING AND GRUBBING	■	EROSION MAT URBAN CLASS I TYPE A
		■	EROSION MAT CLASS I TYPE B



BENCHMARK/CONTROL POINT TABLE						
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION	DESC
14	413+55.71	30171.11 RT	305727.190	853556.597	665.40	CP BM



**BEGIN PROJECT**  
**STA 107+00**  
 MATCH EXISTING  
 BUTT JOINT REQ'D  
 Y=305,802.317  
 X=852,556.083

VAN LOON WILDLIFE AREA

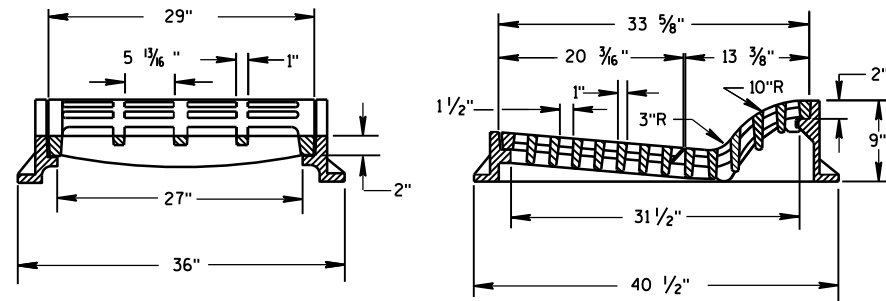
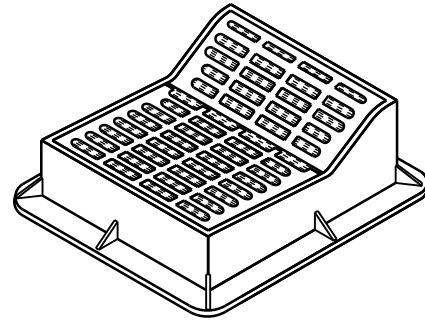
LEGEND			
(A)	INLET PROTECTION TYPE A		SILT FENCE
(C)	INLET PROTECTION TYPE C		WATER FLOW ARROW
- - -	SLOPE INTERCEPT		CULVERT PIPE CHECKS
ΔΔΔ	TEMPORARY DITCH CHECKS		CLEAN DITCH
X	CLEARING AND GRUBBING		EROSION MAT URBAN CLASS I TYPE A
			EROSION MAT CLASS I TYPE B

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## Standard Detail Drawing List

08A05-19C	INLET COVERS TYPE F, HM, HM-S, S, T, V, HM-GJ, & HM-GJ-S
08A05-19D	INLET COVER TYPE BW, MANHOLE COVERS, TYPE K, J, J-S, L & M
08D01-22A	CONCRETE CURB & GUTTER
08D01-22B	CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS
08D05-20A	CURB RAMPS TYPES 1 AND 1-A
08D05-20B	CURB RAMPS TYPES 2 AND 3
08D05-20C	CURB RAMPS TYPES 4A AND 4A1
08D05-20D	CURB RAMPS TYPE 4B AND 4B1
08D05-20E	CURB RAMPS TYPES 5, 6, 7A, 7B & 8
08D05-20F	CURB RAMPS RADIAL DETECTABLE WARNING FIELD APPLICATIONS
08D05-20G	CURB RAMPS RECTANGULAR AND RADIAL DETECTABLE WARNING PLATES
08D22-01	DRIVEWAYS WITHOUT CURB & GUTTER RESURFACING PROJECTS RURAL
08E08-03	TYPICAL INSTALLATIONS OF EROSION BALES / TEMPORARY DITCH CHECKS
08E09-06	SILT FENCE
08E10-02	INLET PROTECTION TYPE A, B, C AND D
08E15-01	CULVERT PIPE CHECK
08F01-11	APRON ENDWALLS FOR CULVERT PIPE
08F04-08	JOINT TIES FOR CONCRETE PIPE AND CONCRETE COLLAR DETAIL
09B02-10	CONDUIT
09C02-09	CONCRETE BASES, TYPES 1, 2, 5, & 6
13A10-02A	2-LANE RURAL SHOULDER RUMBLE STRIP, MILLING
13A10-02B	2-LANE RURAL SHOULDER RUMBLE STRIP, MILLING
13A10-02C	2-LANE RURAL SHOULDER RUMBLE STRIP, MILLING
13A10-02D	2-LANE RURAL SHOULDER RUMBLE STRIP, MILLING
13A11-03A	2-LANE RURAL CENTER LINE RUMBLE STRIP, MILLING
13A11-03B	2-LANE RURAL CENTER LINE RUMBLE STRIP, MILLING
13C19-03	HMA LONGITUDINAL JOINTS
15A03-02A	FLEXIBLE MARKER POST FOR CULVERT END
15A03-02B	FLEXIBLE MARKER POST FOR CULVERT END
15C02-08F	ADVANCED WIDTH RESTRICTION SIGNING
15C03-05	BARRICADES AND SIGNS FOR SIDEROAD CLOSURES
15C04-05	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M. P. H. OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC
15C05-05	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M. P. H. OR LESS
15C07-15A	PAVEMENT MARKING SYMBOLS
15C07-15B	PAVEMENT MARKING WORDS
15C07-15C	PAVEMENT MARKING ARROWS
15C08-22A	LONGITUDINAL MARKING (MAINLINE)
15C08-22B	TEMPORARY LONGITUDINAL PAVEMENT MARKING
15C08-22C	PAVEMENT MARKING (TURN LANES)
15C08-22D	PAVEMENT MARKING (TURN LANES)
15C11-09B	CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS
15C12-09A	TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION
15C12-09B	TRAFFIC CONTROL, LANE CLOSURE WITH AUTOMATED FLAGGER ASSISTANCE DEVICE
15C19-07A	MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY
15C33-04	STOP LINE AND CROSSWALK PAVEMENT MARKING
15C36-01	PARKING STALL MARKING
15D28-04	TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY
15D30-07A	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-07B	TRAFFIC CONTROL, TEMPORARY ADA COMPLIANT PEDESTRIAN ACCOMMODATION
15D30-07C	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-07D	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-07E	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-07F	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-07G	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-07H	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-07I	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-07J	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D39-02	TRAFFIC CONTROL, DROP-OFF SIGNING
15D44-02	TRAFFIC CONTROL, SIGNING ON ROADWAYS WITH MILLED SURFACES
15D45-03	TRAFFIC CONTROL, SIGNING ON ROADWAYS WITH LOOSE GRAVEL
15D48-01	TRAFFIC CONTROL, LANE SHIFT IN FLAGGING OPERATION
15D51-01	TRAFFIC CONTROL, MOBILE OPERATIONS ON AN UNDIVIDED ROADWAY



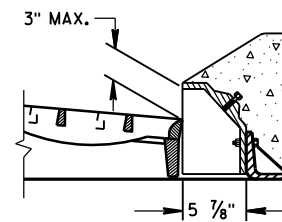
**TYPE "F"**

USE WITH TYPES A & D CONCRETE CURB & GUTTER, 36 INCH.

**GENERAL NOTES**

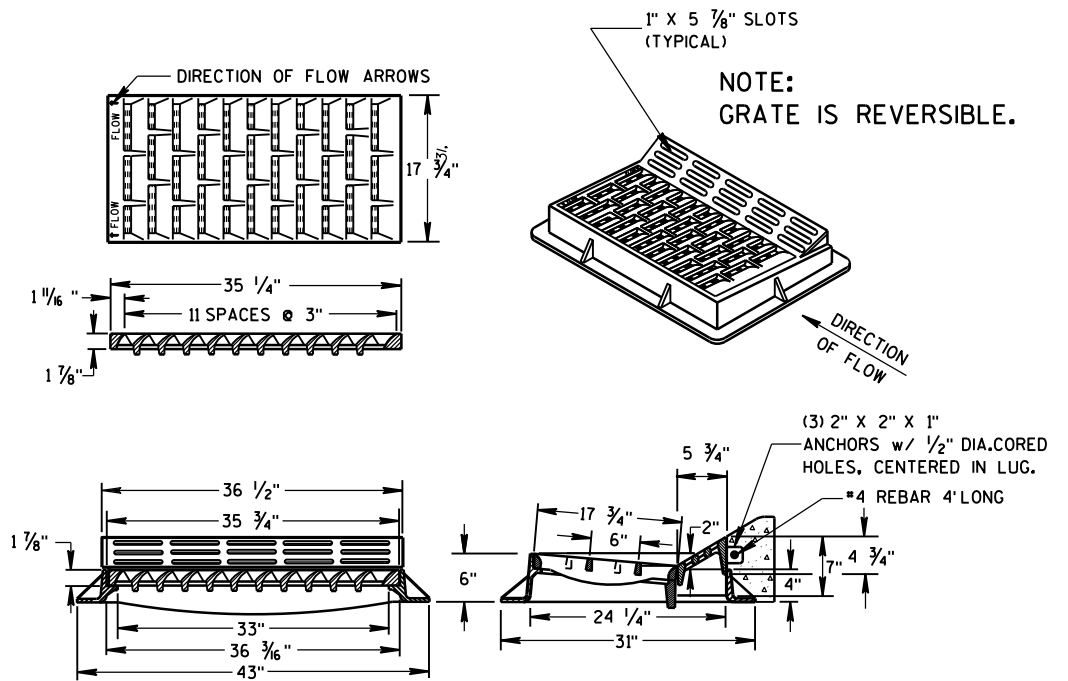
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

DETAIL DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR INLET COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.



**ALTERNATIVE CURB BOX FOR TYPE "HM" COVER**

USE WITH TYPES G & J CONCRETE CURB & GUTTER, 30 INCH NOTED AS TYPE HM-GJ ON DRAINAGE TABLE



**TYPE "HM"**

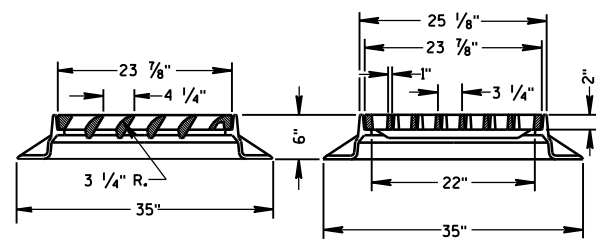
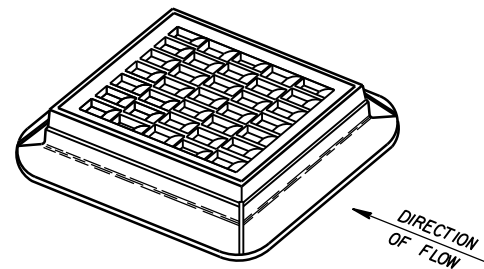
USE WITH TYPES A & D CONCRETE CURB & GUTTER, 36 INCH.

NOTE: SPECIAL GRATE FOR THE TYPE "H" COVER MAY ALSO BE USED FOR THE TYPE "HM" COVER NOTED AS TYPE HM-S ON DRAINAGE TABLE

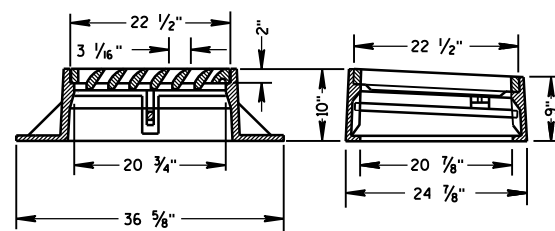
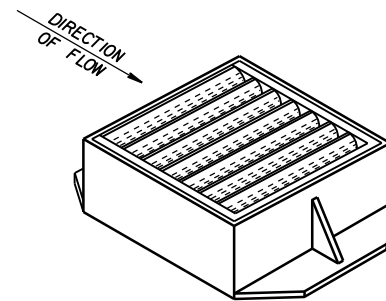
NOTE: SPECIAL GRATE FOR THE TYPE "H" COVER MAY ALSO BE USED FOR THE TYPE "HM-GJ" COVER NOTED AS TYPE HM-GJ-S ON DRAINAGE TABLE

6

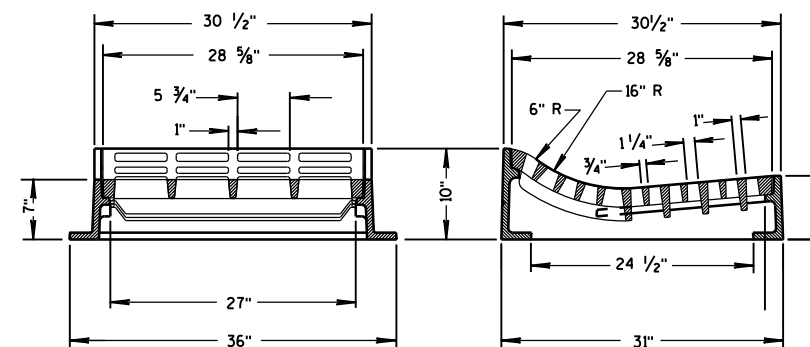
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**TYPE "S"**

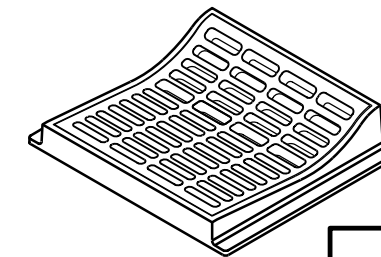


**TYPE "V"**



**TYPE "T"**

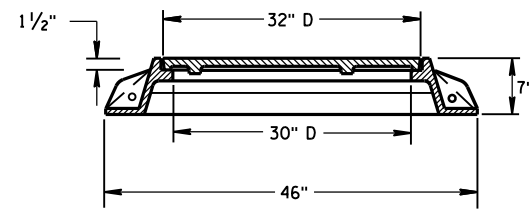
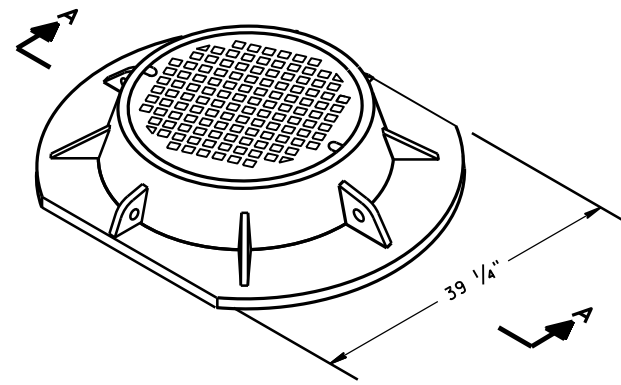
USE WITH TYPES R & T CONCRETE CURB & GUTTER, 36 INCH.



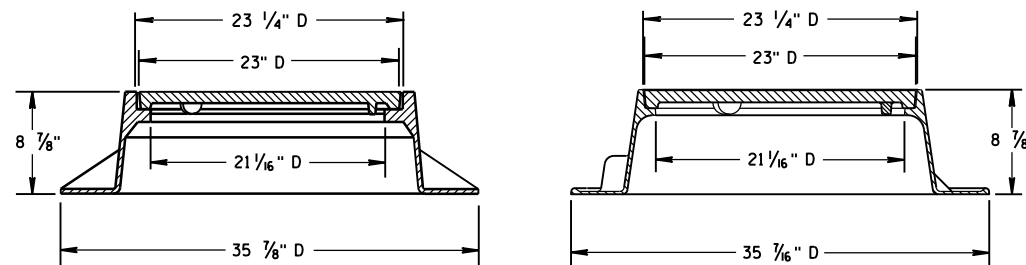
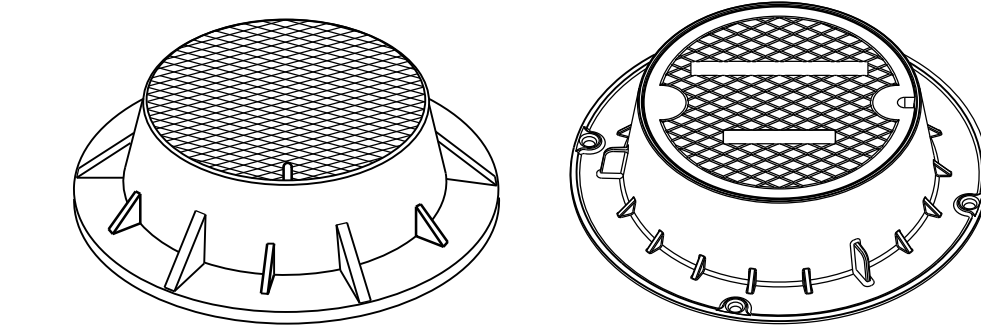
**INLET COVERS**  
TYPE F, HM, HM-S, S, T, V,  
HM-GJ, & HM-GJ-S

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
11/27/2013 DATE /s/ Jerry H. Zogg  
ROADWAY STANDARDS DEVELOPMENT ENGINEER  
FHWA



SECTION A-A  
TYPE "K"



TYPE "J"

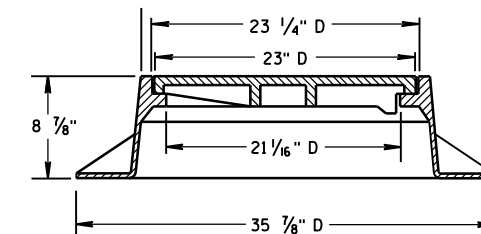
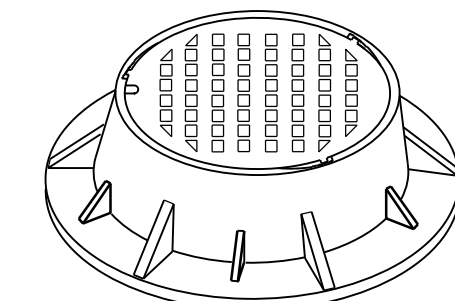
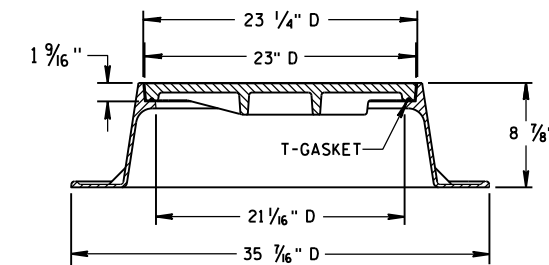
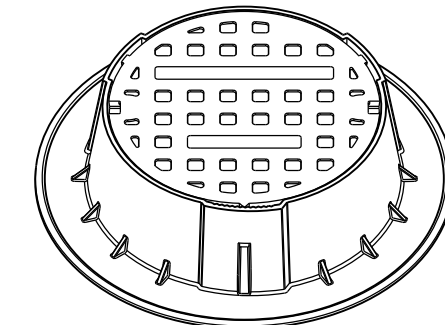
NOTE: EITHER CASTING IS ACCEPTABLE

**GENERAL NOTES**

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

DETAIL DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR MANHOLE COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ROUND FRAMES AND COVERS SHALL HAVE CONTINUOUSLY MACHINED BEARING SURFACES TO PREVENT ROCKING AND RATTLING.



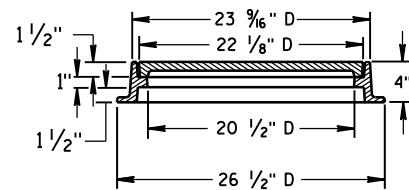
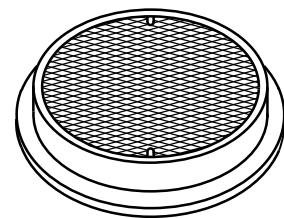
TYPE "J" SPECIAL

TYPE "B" NON-ROCKING SELF-SEAL LID

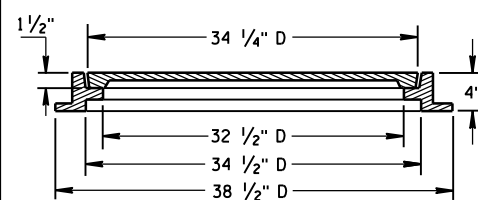
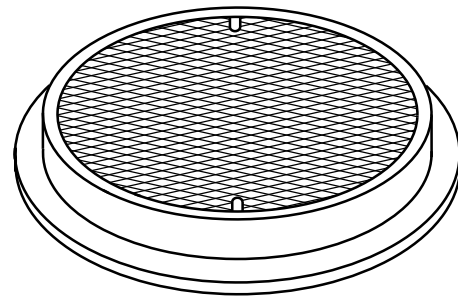
(NOTED AS TYPE J-S ON THE DRAINAGE TABLE)

NOTE: EITHER CASTING IS ACCEPTABLE

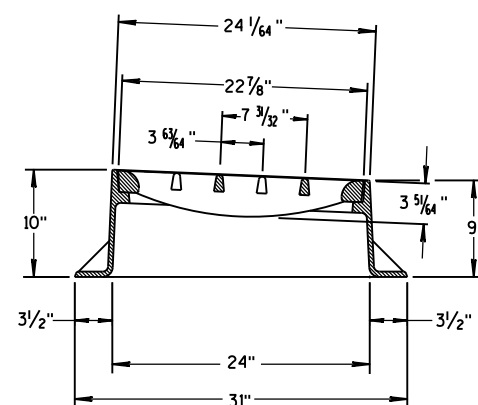
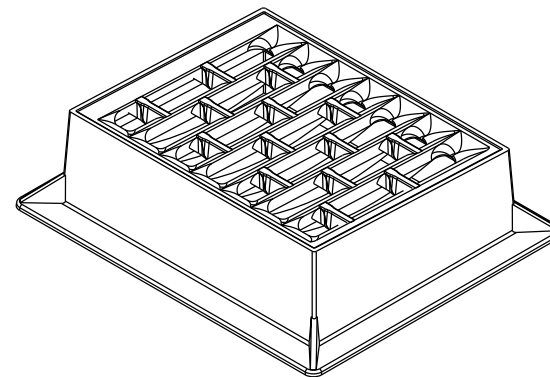
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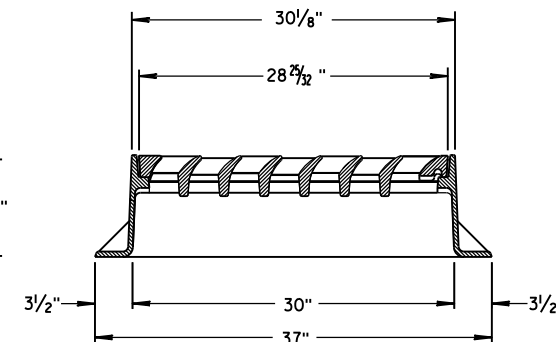
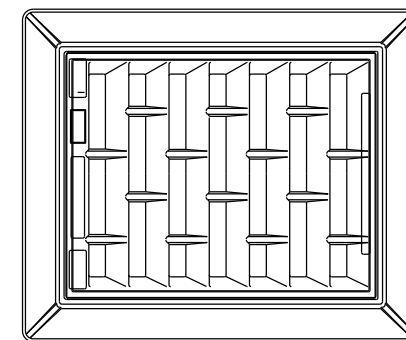
TYPE "L"



TYPE "M"



INLET COVER TYPE "BW"



6

S.D.D. 8 A 5-19d

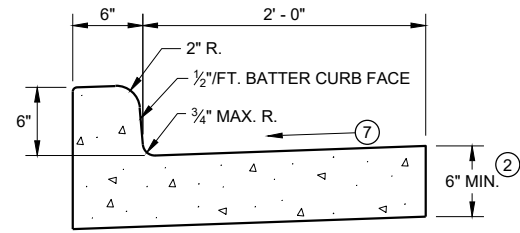
S.D.D. 8 A 5-19d

INLET COVER TYPE BW  
MANHOLE COVERS, TYPE K,  
J, J-S, L & M

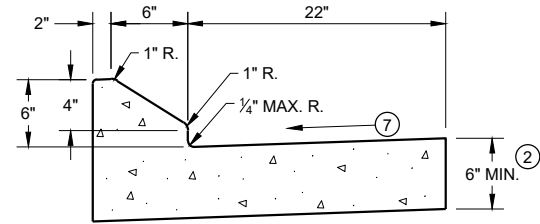
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
11/27/2013  
DATE  
FHWA

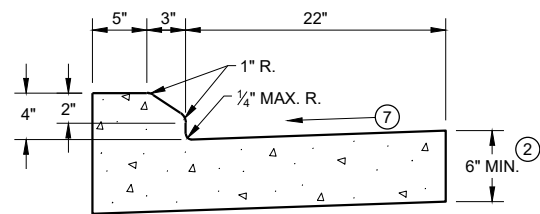
/s/ Jerry H. Zogg  
ROADWAY STANDARDS DEVELOPMENT  
ENGINEER



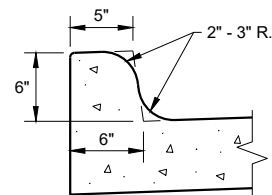
TYPES A<sup>①</sup> & D



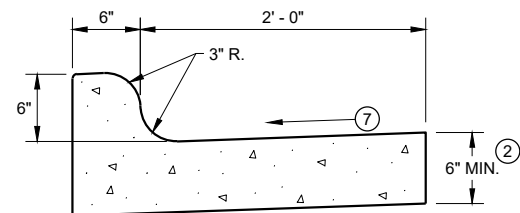
6" SLOPED CURB TYPES G<sup>①</sup> & J



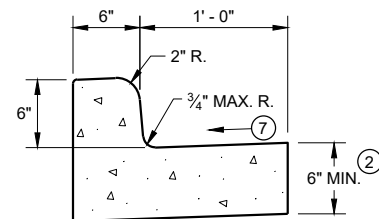
4" SLOPED CURB TYPES G<sup>①</sup> & J



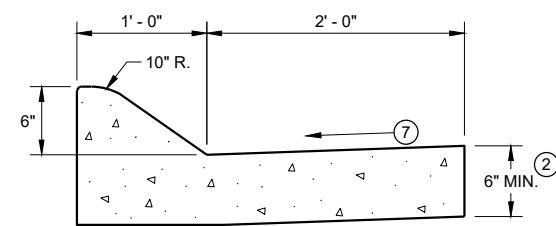
TYPES K<sup>①</sup> & L  
(OPTIONAL CURB SHAPE)



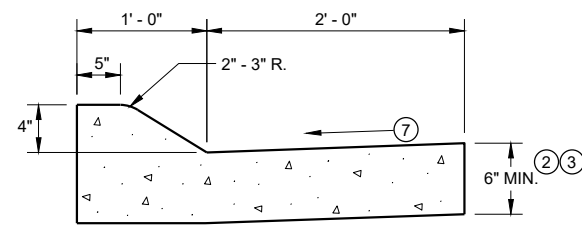
TYPES K<sup>①</sup> & L  
CONCRETE CURB AND GUTTER 30"



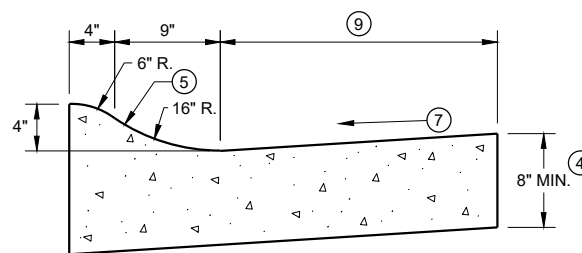
TYPES A<sup>①</sup> & D  
CONCRETE CURB AND GUTTER 18"



6" SLOPED CURB TYPES A<sup>①</sup> & D

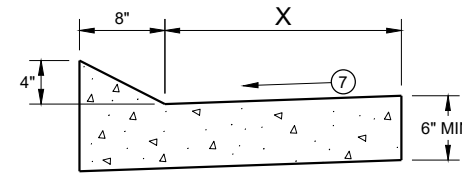


4" SLOPED CURB TYPES A<sup>①</sup> & D  
CONCRETE CURB AND GUTTER 36"



4" SLOPED CURB TYPES R<sup>①</sup> & T

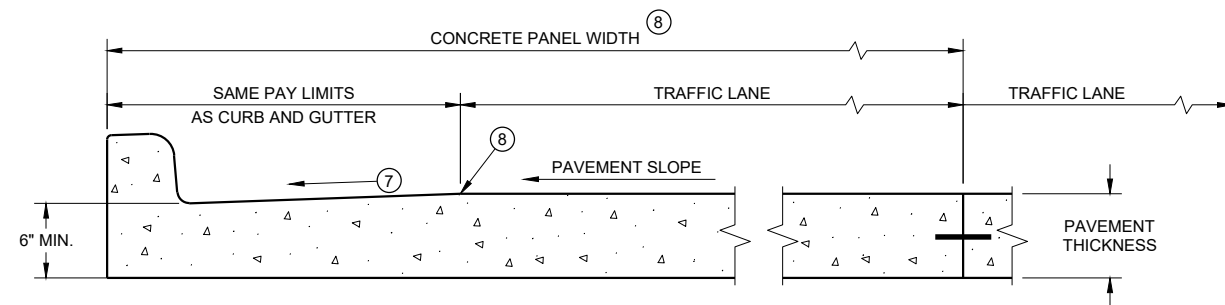
TBT & TBTT	X
30"	22"
36"	28"



TYPES TBT & TBTT<sup>①</sup>  
CONCRETE CURB AND GUTTER

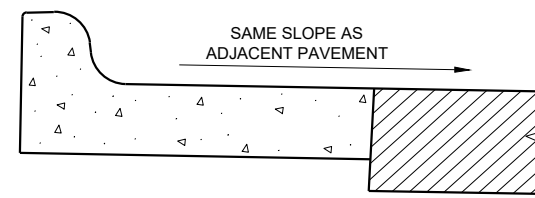
PAVEMENT THICKNESS  
AND MAXIMUM CONCRETE  
PANEL WIDTH TABLE

PAVEMENT THICKNESS	MAXIMUM PANEL WIDTH
LESS THAN 10"	12'
10" & ABOVE	15'



PARTIAL SECTION OF PAVEMENT \*  
WITH INTEGRAL CURB AND GUTTER

\* BIKE LANE IS NOT SHOWN



REVERSE SLOPE GUTTER<sup>⑥</sup>  
(TYPICAL FOR ALL CURB & GUTTER TYPES)

GENERAL NOTES

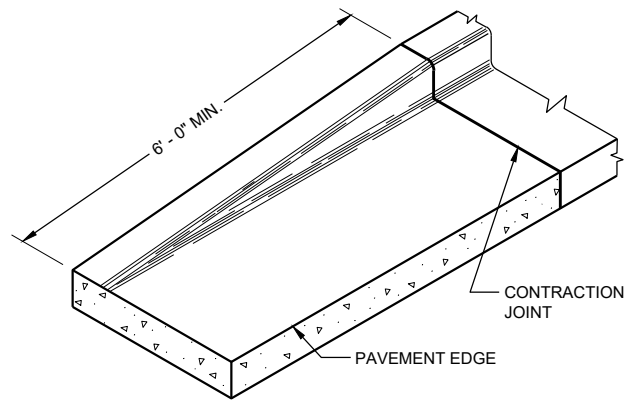
DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

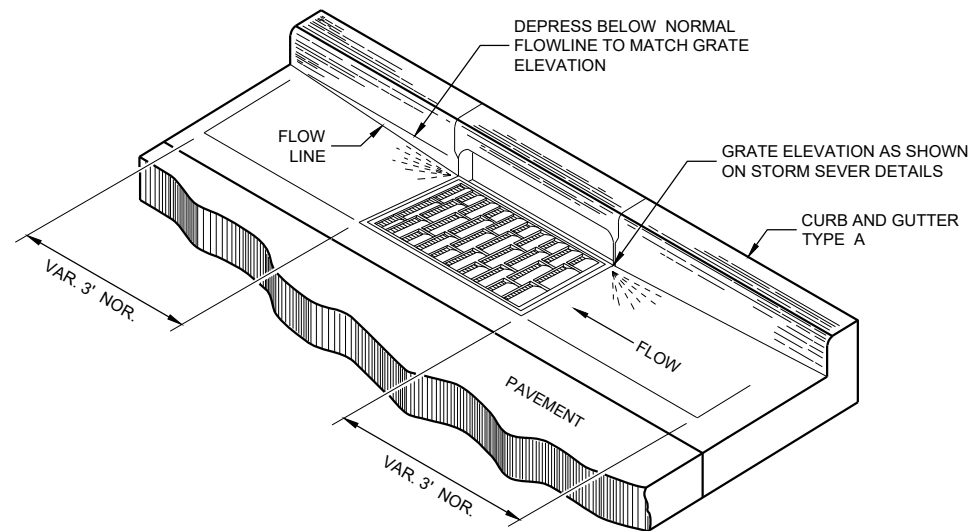
INTEGRAL CURB AND GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB AND GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ③ USE 8" MINIMUM GUTTER THICKNESS WHEN USED WITH AN ADJACENT CONCRETE TRUCK APRON PLACED BEHIND BACK OF CURB.
- ④ THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑤ UNLESS OTHERWISE NOTED, FOR STAKING PURPOSES THE FACE OF CURB IS 6" FROM THE BACK OF CURB.
- ⑥ WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.
- ⑦ USE 4% GUTTER CROSS SLOPE UNLESS OTHERWISE NOTED IN THE PLANS.
- ⑧ INCLUDE LONGITUDINAL JOINT AND TIE BARS ALONG LANE EDGE WHEN CONCRETE PANEL WIDTH EXCEEDS THE MAXIMUM WIDTH PER TABLE BELOW. LONGITUDINAL JOINT(S) ARE NOT ALLOWED WITHIN TRAFFIC LANES AND BIKE LANES. LONGITUDINAL JOINT MAY BE SAWED.
- ⑨ CONCRETE CURB AND GUTTER 4-INCH SLOPED 30-INCH TYPE "R" AND "T" = 17 INCHES  
CONCRETE CURB AND GUTTER 4-INCH SLOPED 36-INCH TYPE "R" AND "T" = 23 INCHES



**END SECTION CURB AND GUTTER**



**DETAIL OF CURB AND GUTTER AT INLETS**  
(TYPICAL H INLET COVER SHOWN)

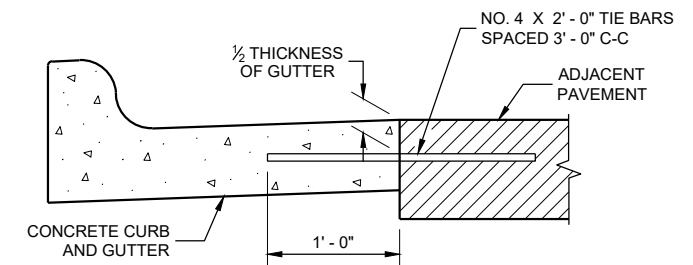
**GENERAL NOTES**

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

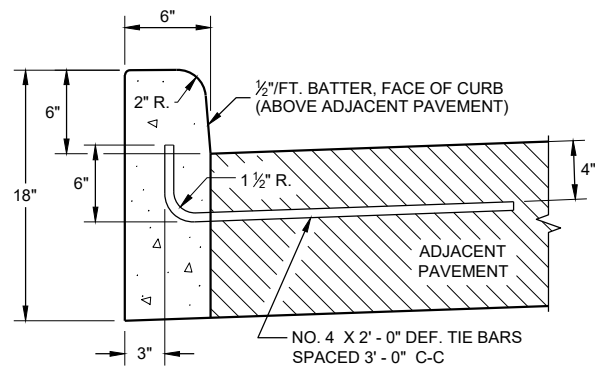
PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2'-0" BEHIND THE BACK OF CURBS.

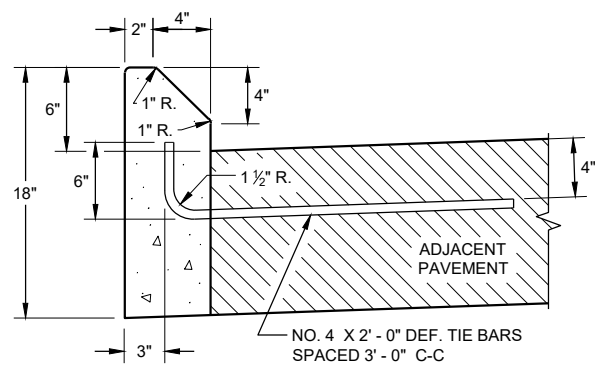
- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑨ REFER TO SDD 08D18 AND 08D19 FOR ADDITIONAL DRIVEWAY ENTRANCE CURB DETAILS.



**TYPICAL TIE BAR LOCATION** ①

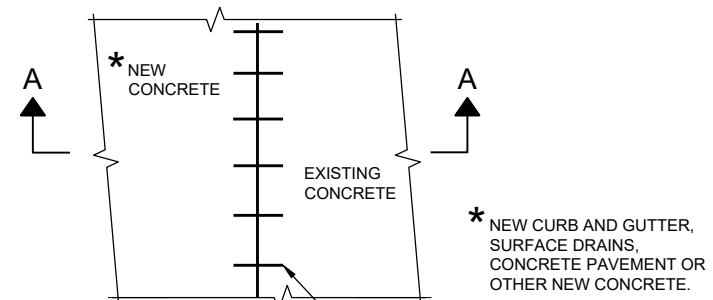


**TYPES A ① & D**

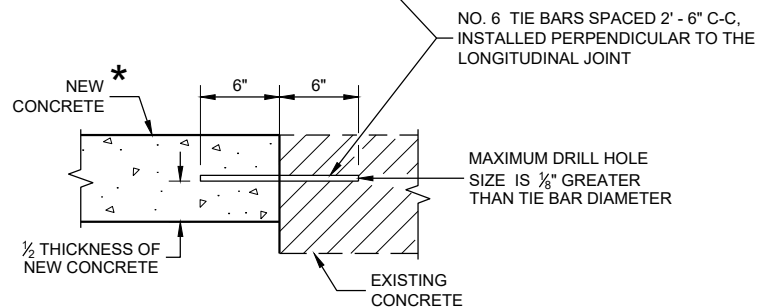


**TYPES G ① & J**

**CONCRETE CURB**

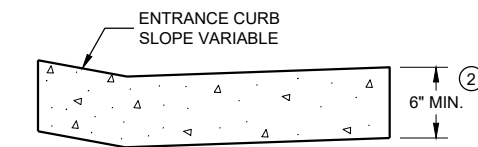


**PLAN VIEW**



**SECTION A - A**

**TIE BARS DRILLED INTO EXISTING PAVEMENT**



**DRIVEWAY ENTRANCE CURB** ⑨  
(WHEN DIRECTED BY THE ENGINEER)

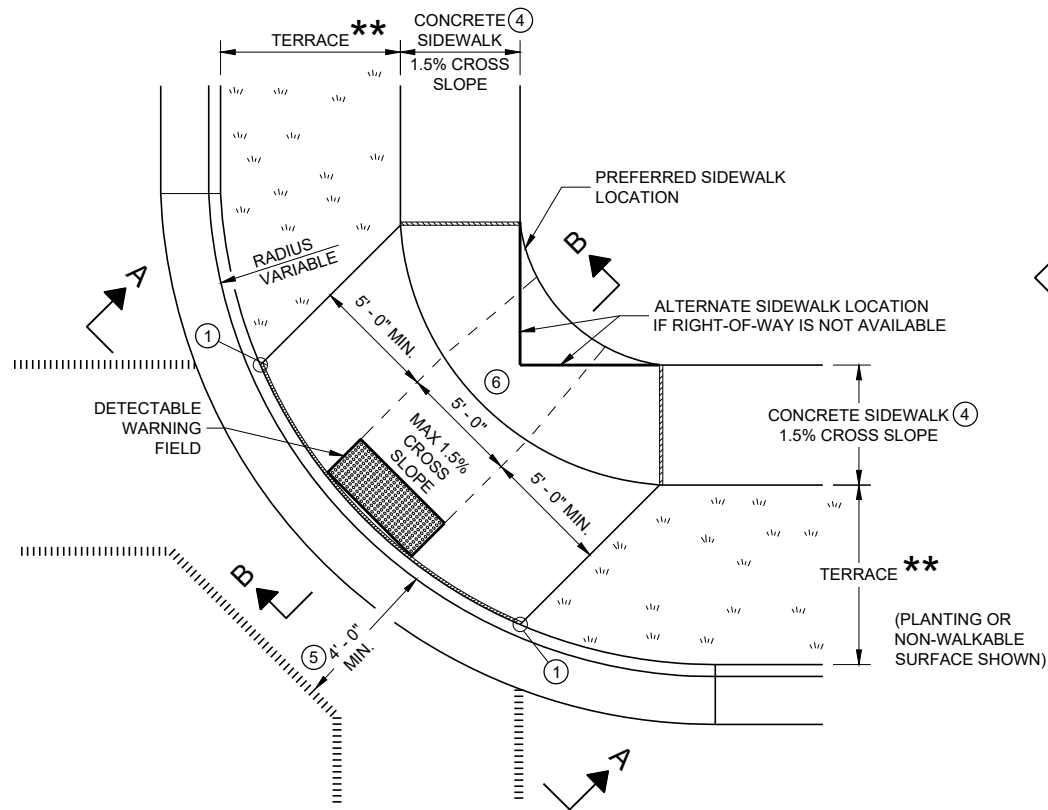
**CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

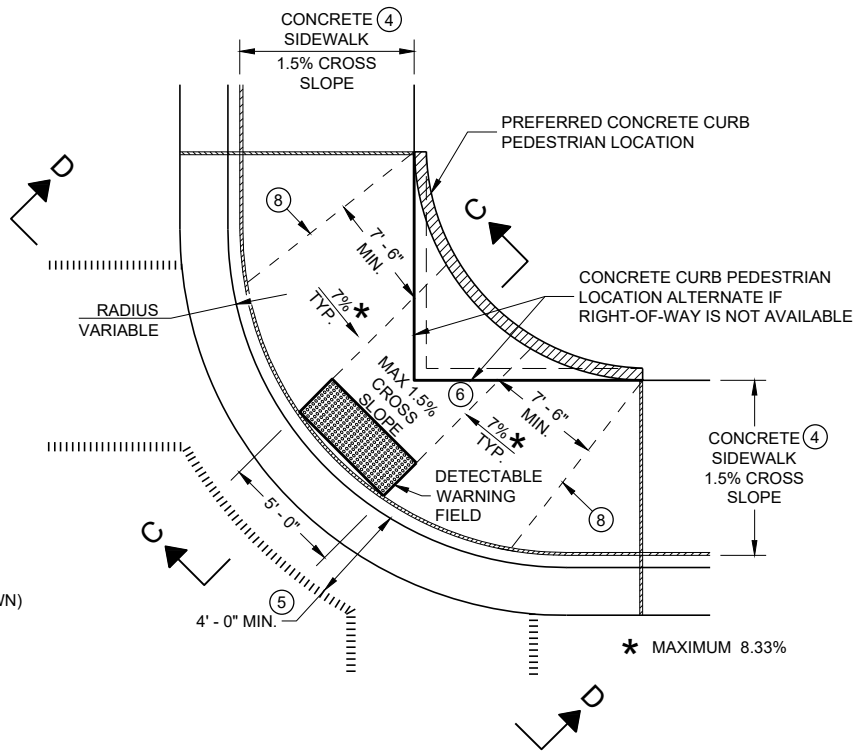
APPROVED  
February 2021 /S/ Rodney Taylor  
DATE ROADWAY STANDARDS DEVELOPMENT ENGINEER

FHWA

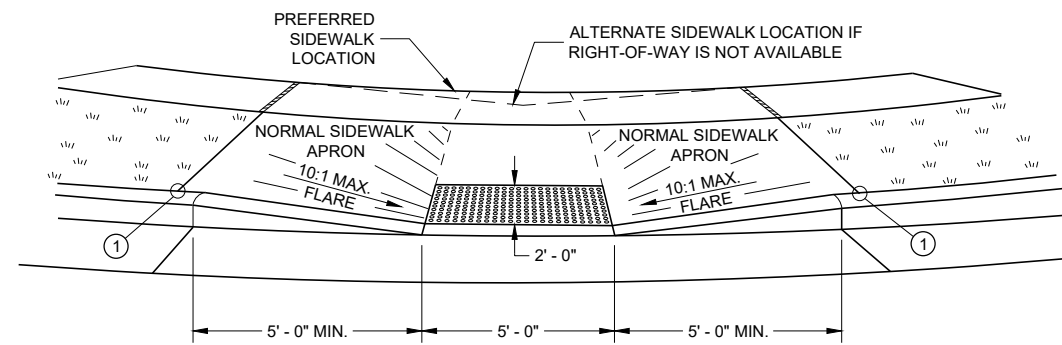




**PLAN VIEW  
CURB RAMP TYPE 1  
(CENTER OF CORNER RADIUS)**

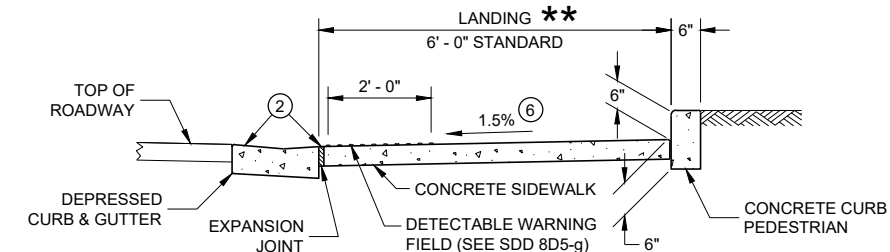


**PLAN VIEW  
CURB RAMP TYPE 1 - A  
(NO TERRACE)**

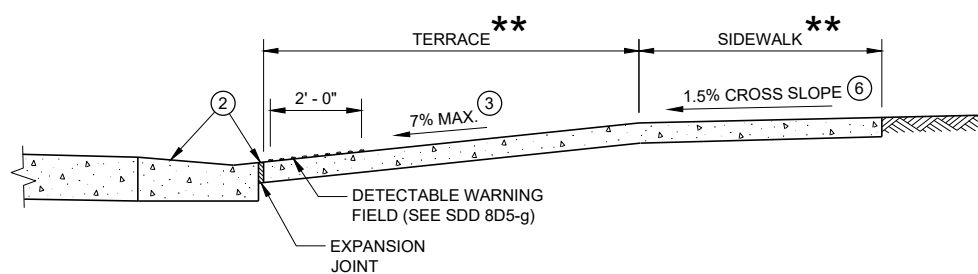


**VIEW A - A FOR TYPE 1**

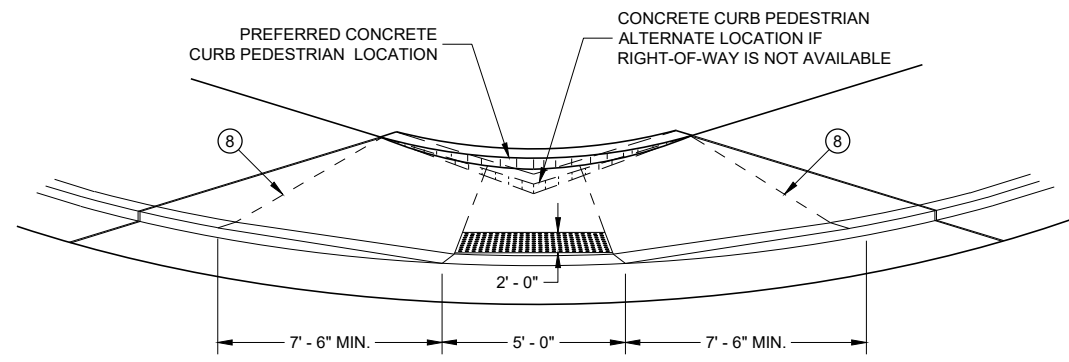
\*\* WIDTH SHOWN ELSEWHERE  
IN THE PLANS



**SECTION C - C FOR TYPE 1 - A**



**SECTION B - B FOR TYPE 1**



**VIEW D - D FOR TYPE 1 - A**

**GENERAL NOTES**

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.  
 DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

WHEN NECESSARY, THE SIDEWALK ELEVATION MAY BE LOWERED TO MEET THE HIGH POINT ON THE RAMP.  
 TYPE 1 CURB RAMPS SHALL HAVE A NORMAL SIDEWALK APRON AND CURB ON BOTH SIDES OF RAMP.

DETECTABLE WARNING FIELD SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS "CURB RAMP DETECTABLE WARNING FIELD". THE CONCRETE PEDESTRIAN CURB, IF NEEDED, SHALL BE MEASURED AND PAID BY THE LINEAR FOOT AS "CONCRETE CURB PEDESTRIAN". CONCRETE SIDEWALK IN THE CURB RAMP AREA SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS CONCRETE SIDEWALK, INCLUDING THE AREA UNDER THE DETECTABLE WARNING FIELD.

SELECT CURB RAMP DETECTABLE WARNING FIELD MATERIALS AND DEVICES FROM THE DEPARTMENT'S APPROVED MATERIALS LIST. THE COLOR OF THE DETECTABLE WARNING FIELD IS SPECIFIED ELSEWHERE AND IS INCIDENTAL TO THE BID ITEM OF "CURB RAMP DETECTABLE WARNING FIELD"

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

SURFACE TEXTURE OF THE RAMP SHALL BE OBTAINED BY COARSE BROOMING TRANSVERSE TO THE SLOPE OF THE RAMP.

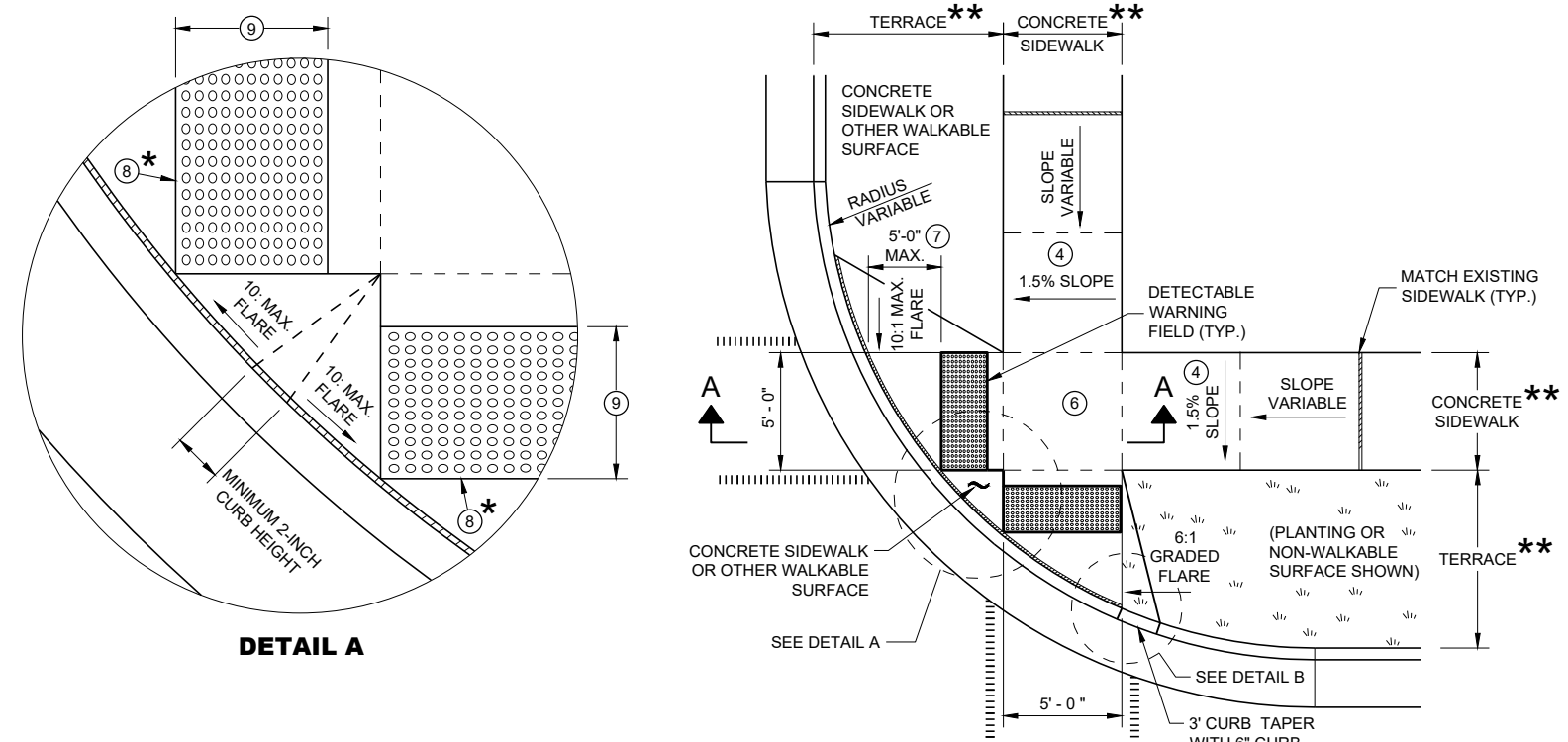
- ① THIS POINT IS AN EXTENSION OF OUTSIDE EDGE OF APPROACHING SIDEWALK WHERE IT MEETS THE BACK OF CONCRETE CURB. POINT LOCATION MAY BE ADJUSTED TO ALIGN WITH BEGINNING OF FULL-HEIGHT CURB IF THIS DISTANCE IS SHORT.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ MAXIMUM 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑤ PROVIDE A LEVEL LANDING IN THE STREET AND GUTTER AREA (2% MAXIMUM SLOPE IN ANY DIRECTION). WHEN THE GUTTER SLOPE EXCEEDS 2%, CONSTRUCT THE LEVEL LANDING IN THE STREET AREA.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

**LEGEND**

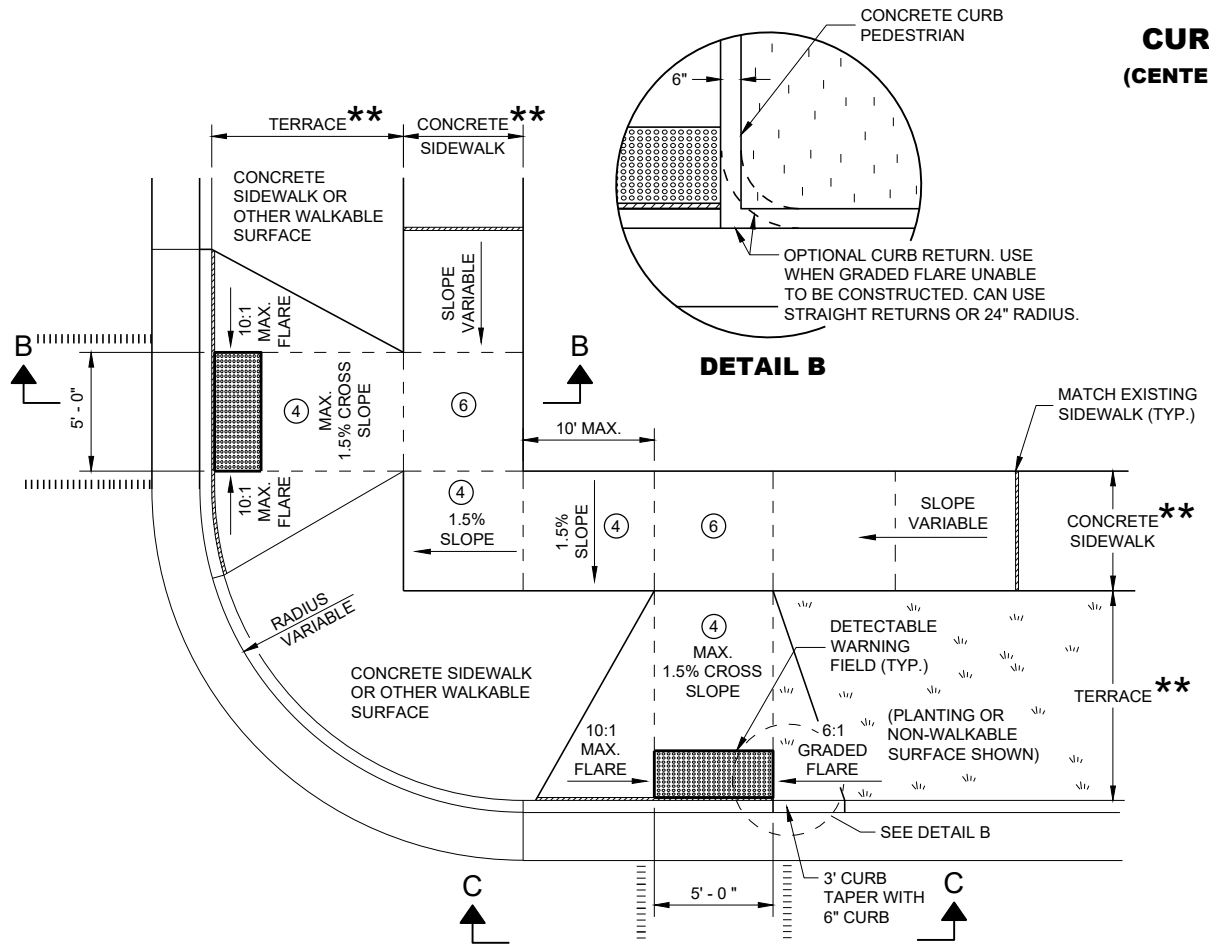
- 1/2" EXPANSION JOINT SIDEWALK
- - - - - CONTRACTION JOINT FIELD LOCATED
- ||||||| PAVEMENT MARKING CROSSWALK (WHITE)

**CURB RAMPS  
TYPE 1 AND 1-A**

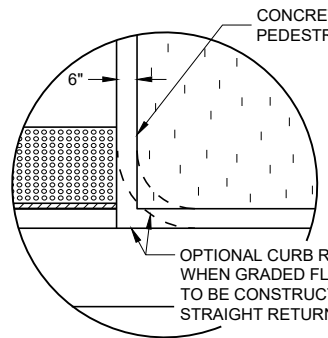
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION



**PLAN VIEW CURB RAMP TYPE 2 (CENTER OF CORNER RADIUS)**



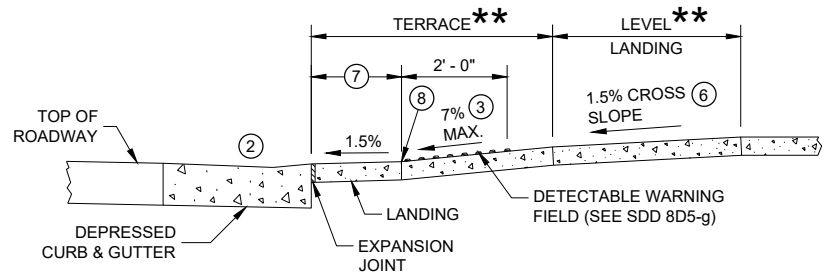
**PLAN VIEW CURB RAMP TYPE 3 (OUTSIDE OF CROSSWALK AREA)**



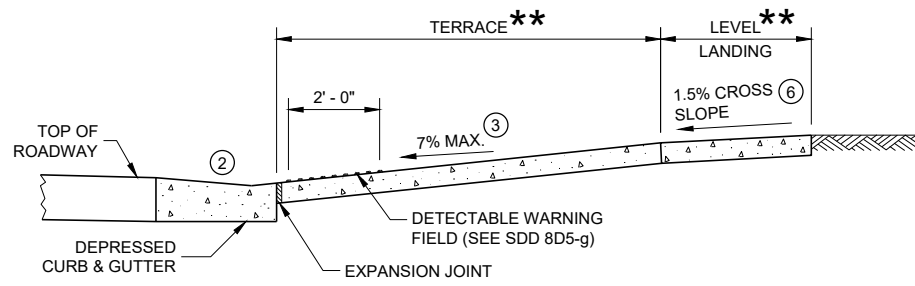
**DETAIL B**

**GENERAL NOTES**

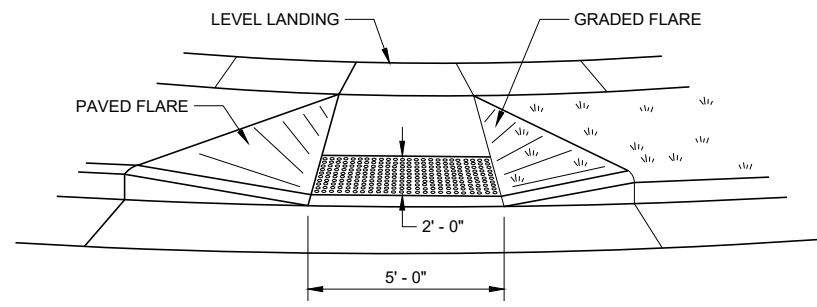
- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE (2.67% OR LESS) AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET X 5 FEET.
- ⑦ WHEN GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑨ WHEN DISTANCE IS LESS THAN 6' - 0", IT MAY BE DIFFICULT TO ACHIEVE A 7% SLOPE OR FLATTER ALONG THE RAMP. REDUCE CURB HEIGHT IN TRIANGLE AREA TO ACHIEVE 7% SLOPE OR FLATTER ON RAMP. CONSTRUCT 2-INCH MINIMUM CURB HEIGHT BETWEEN 10:1 FLARES.



**SECTION A - A FOR TYPE 2**



**SECTION B - B FOR TYPE 3**



**VIEW C - C FOR TYPE 3**

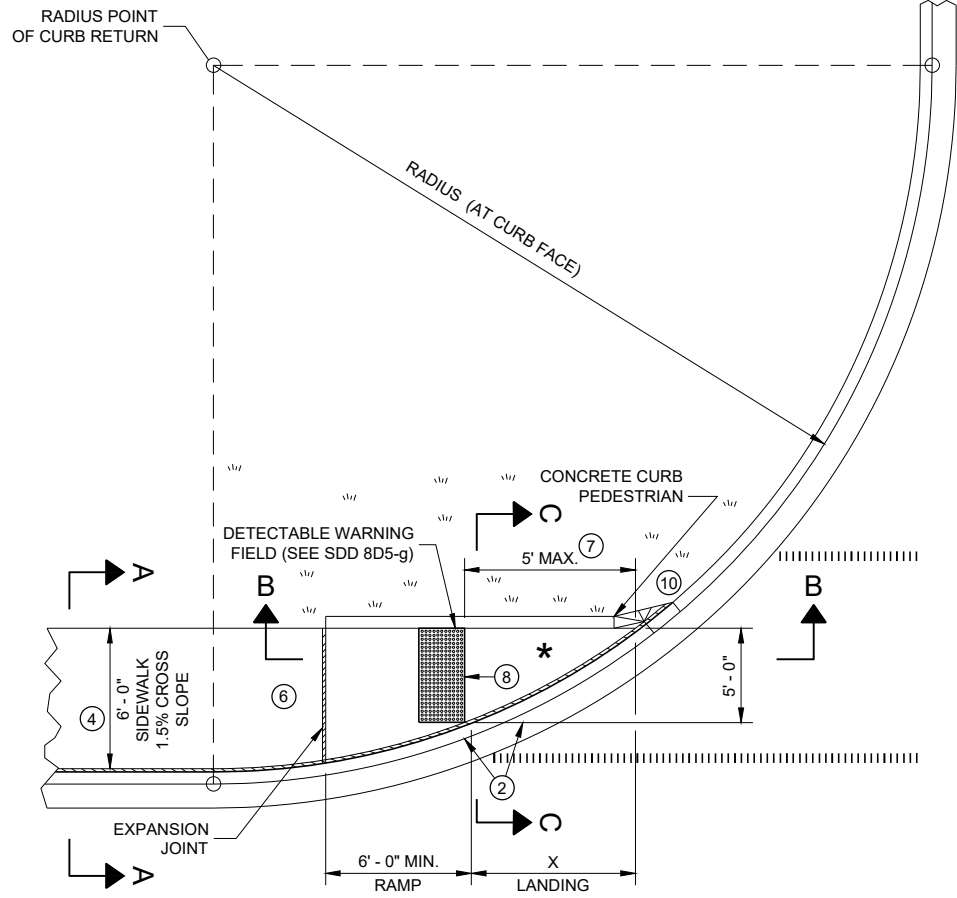
- \* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK
- \*\* WIDTH SHOWN ELSEWHERE IN THE PLANS

**LEGEND**

- 1/2" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT SIDEWALK
- PAVEMENT MARKING CROSSWALK (WHITE)

**CURB RAMPS  
TYPE 2 AND 3**

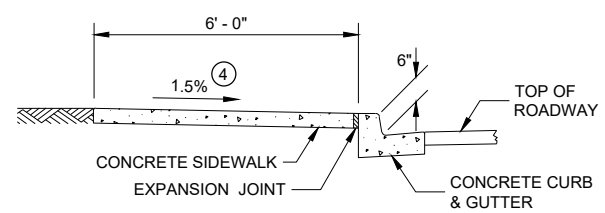
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION



**PLAN VIEW  
CURB RAMP TYPE 4A**

RADIUS (AT CURB FACE)	X
10 FEET	4' - 7"
15 FEET	6' - 5 1/2"

INTERMEDIATE RADII CAN BE INTERPOLATED



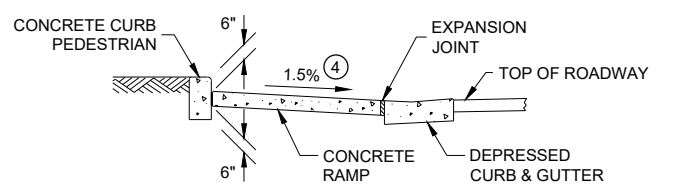
**SECTION A - A FOR TYPE 4A**

**GENERAL NOTES**

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑦ WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑩ INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

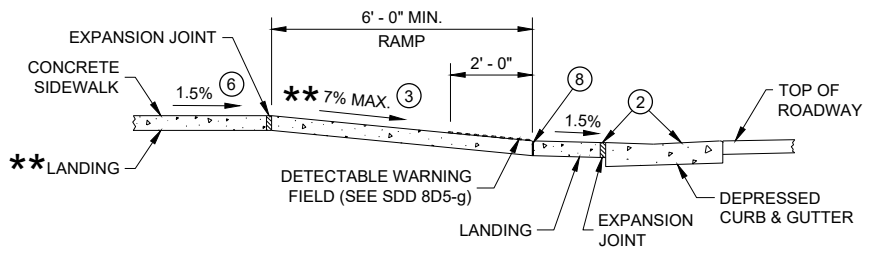
**LEGEND**

- 1/2" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT SIDEWALK
- PAVEMENT MARKING CROSSWALK (WHITE)



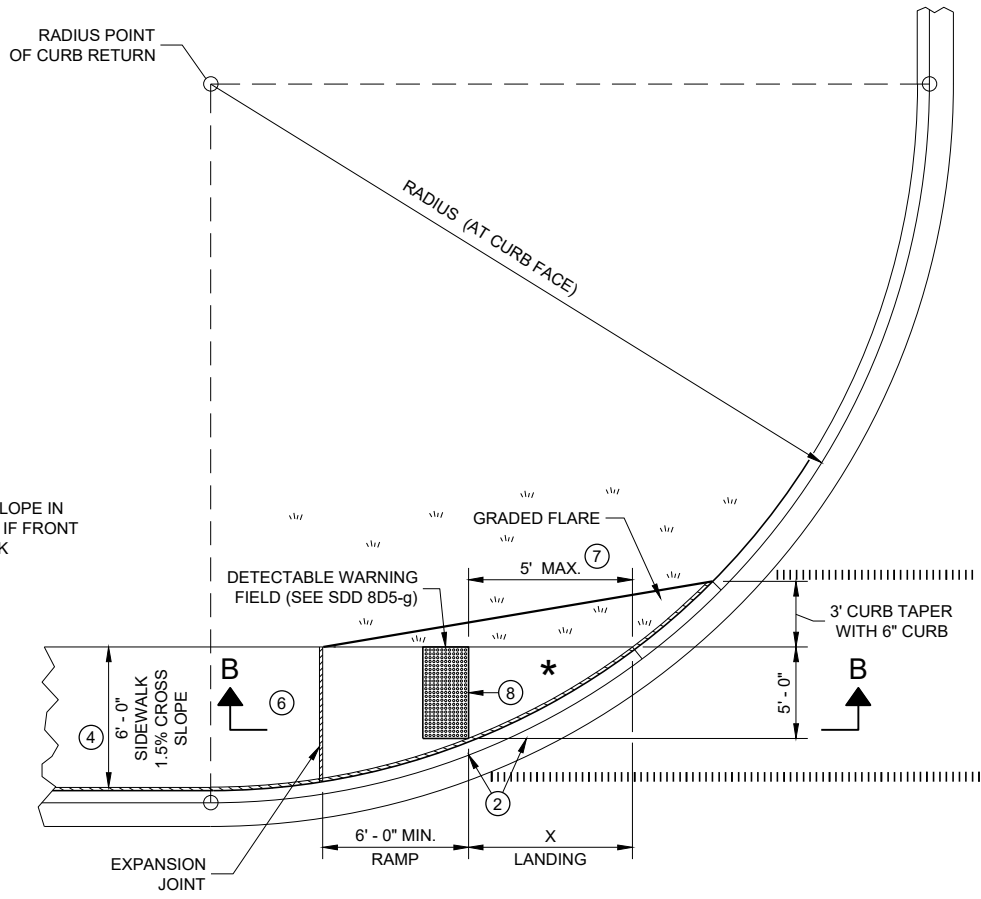
**SECTION C - C FOR TYPE 4A**

\* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IF FRONT OF GRADE BREAK

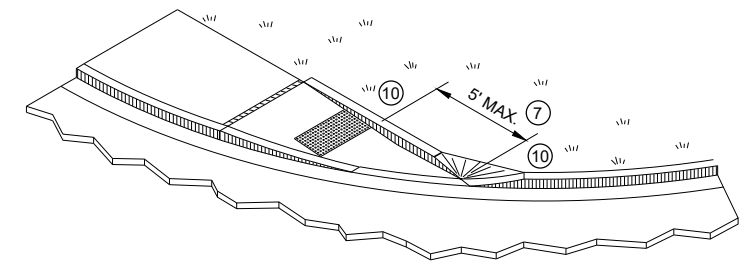


**SECTION B - B FOR  
TYPE 4A AND TYPE 4A1**

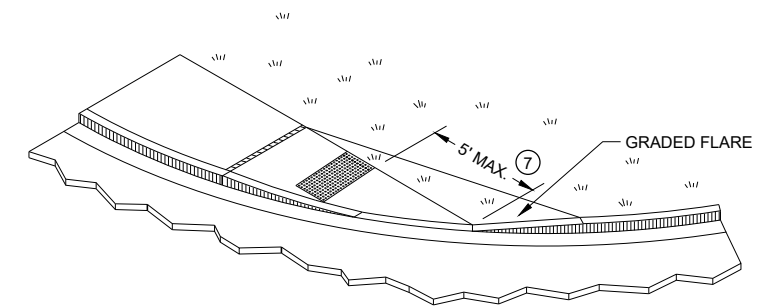
\*\* IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED



**PLAN VIEW  
CURB RAMP TYPE 4A1**



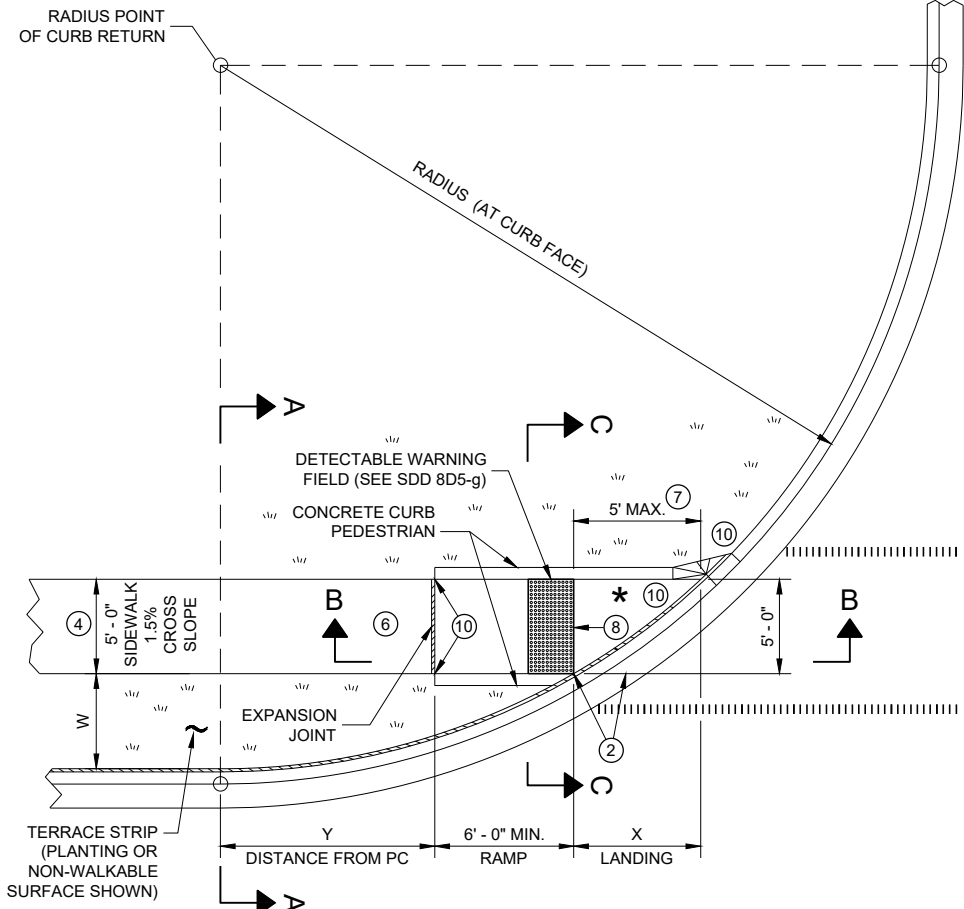
**ISOMETRIC VIEW FOR TYPE 4A**



**ISOMETRIC VIEW FOR TYPE 4A1**

**CURB RAMPS  
TYPE 4A AND 4A1**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION



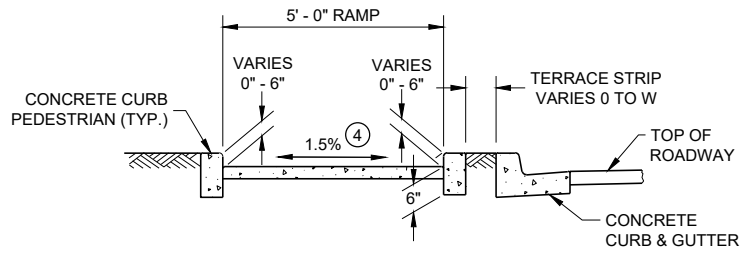
RADIUS (AT CURB FACE)	W = 3' - 0"		W = 4' - 0"		W = 5' - 0"		W = 6' - 0"		W = 7' - 0"		W = 8' - 0"		W = 9' - 0"		W = 10' - 0"	
	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y
10 FEET	2' - 10 1/4"	0' - 5"	2' - 1"	1' - 4 1/2"	1' - 5"	2' - 1"	0' - 10"	2' - 7 1/2"	0' - 3 1/4"	3' - 0 1/4"						
15 FEET	4' - 6 3/4"	2' - 1 3/4"	3' - 9"	3' - 5 3/4"	3' - 1 1/4"	4' - 6"	2' - 6 3/4"	5' - 4 1/2"	2' - 1"	6' - 1"	1' - 8"	6' - 8 1/2"	1' - 3 1/4"	7' - 2 1/2"	0' - 10 3/4"	7' - 7 1/4"
20 FEET	5' - 9 3/4"	3' - 6 1/2"	4' - 11 1/2"	5' - 1 3/4"	4' - 3 3/4"	6' - 5 1/2"	3' - 8 3/4"	7' - 7"	3' - 3"	8' - 6 1/2"	2' - 10"	9' - 4 1/2"	2' - 5 1/2"	10' - 1 1/4"	2' - 1 1/4"	10' - 9"
30 FEET			6' - 9 1/4"	7' - 11 1/4"	6' - 0 1/4"	9' - 8"	5' - 5"	11' - 1 3/4"	4' - 10 3/4"	12' - 5 3/4"	4' - 5 1/2"	13' - 7 3/4"	4' - 0 3/4"	14' - 8 1/2"	3' - 8 1/2"	15' - 8 1/4"
40 FEET									6' - 1 3/4"	15' - 8 1/2"	5' - 8"	17' - 2"	5' - 3"	18' - 5 3/4"	4' - 10 3/4"	19' - 8 1/4"
50 FEET															5' - 10 1/4"	23' - 2"

INTERMEDIATE RADII CAN BE INTERPOLATED  
 DIMENSION "Y" IS CALCULATED BASED ON 6'-0" RAMP LENGTH  
 DIMENSION "X" IS CALCULATED BASED ON 5'-0" SIDEWALK WIDTH

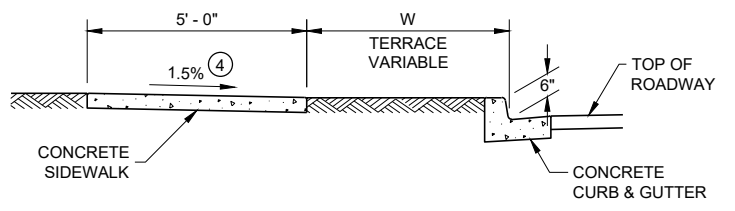
- LEGEND**
- ===== 1/2" EXPANSION JOINT SIDEWALK
  - - - - - CONTRACTION JOINT SIDEWALK
  - ||||||| PAVEMENT MARKING CROSSWALK (WHITE)

**GENERAL NOTES**

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- (2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/2 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- (3) AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- (4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- (6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- (7) WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- (8) PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- (10) INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

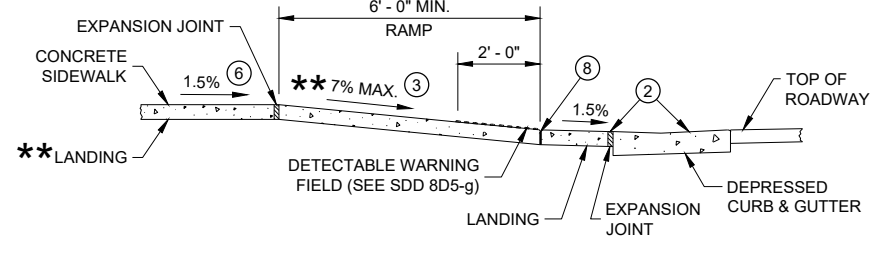


**SECTION C - C FOR TYPE 4B**



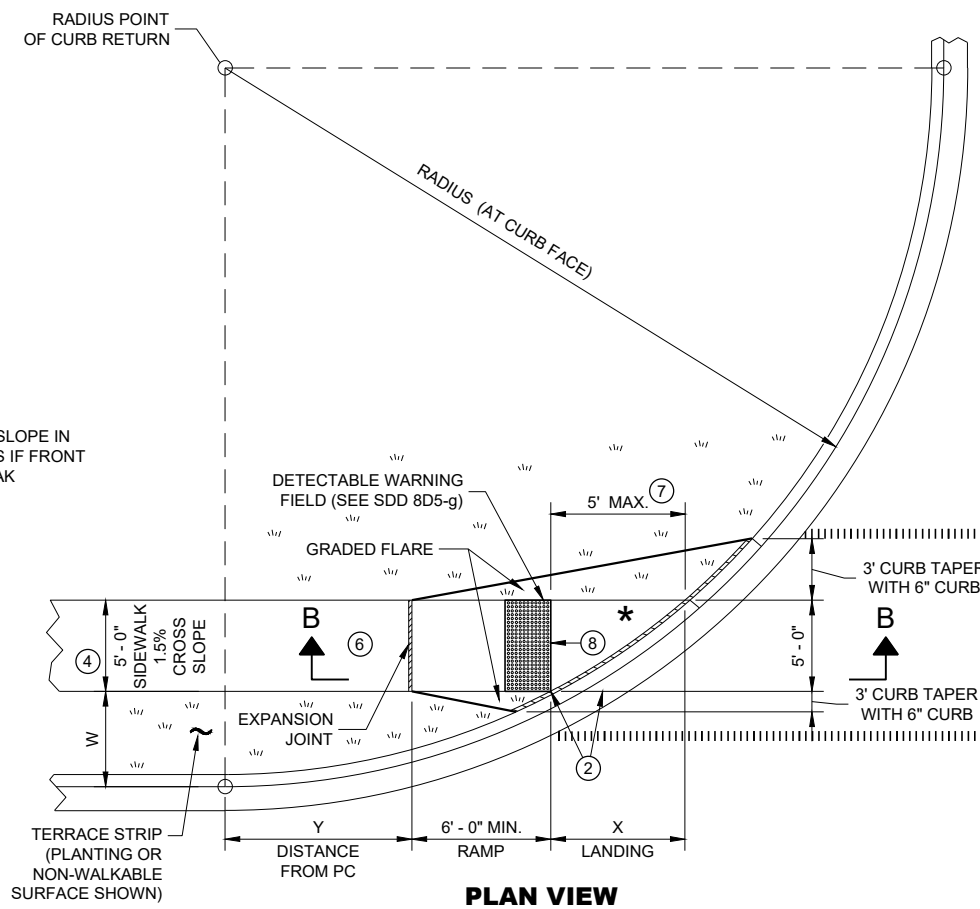
**SECTION A - A FOR TYPE 4B**

\* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IF FRONT OF GRADE BREAK

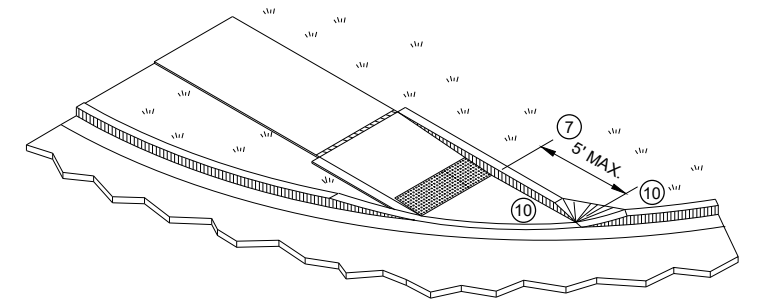


**SECTION B - B FOR TYPE 4B AND TYPE 4B1**

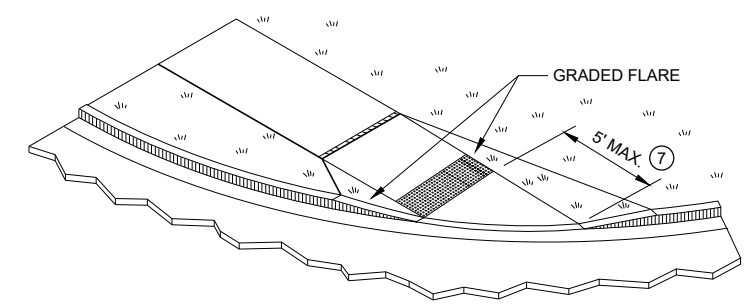
\*\* IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED



**PLAN VIEW CURB RAMP TYPE 4B1**



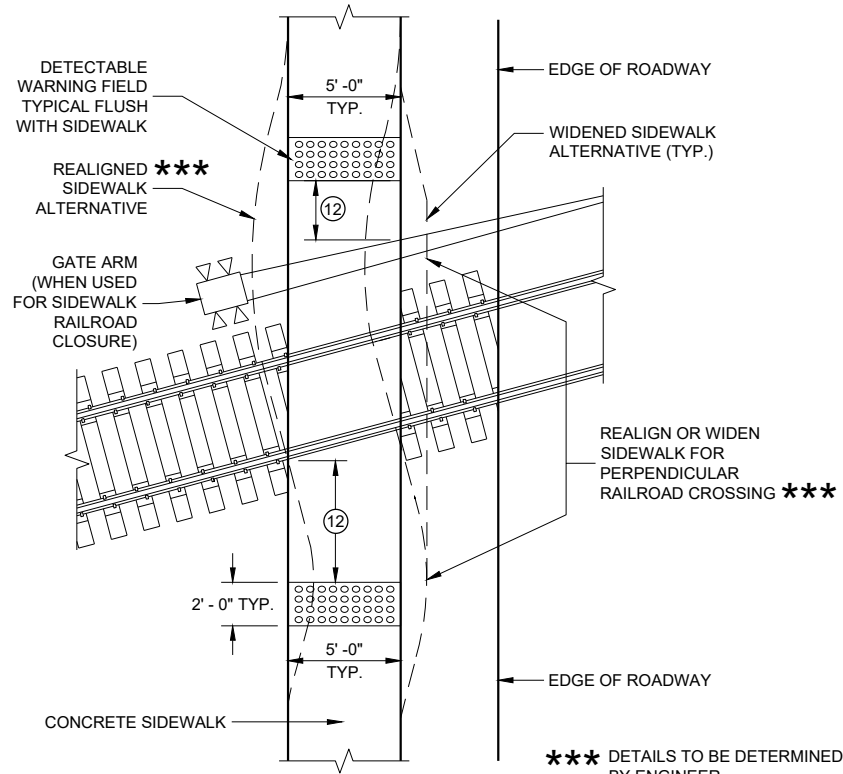
**ISOMETRIC VIEW FOR TYPE 4B**



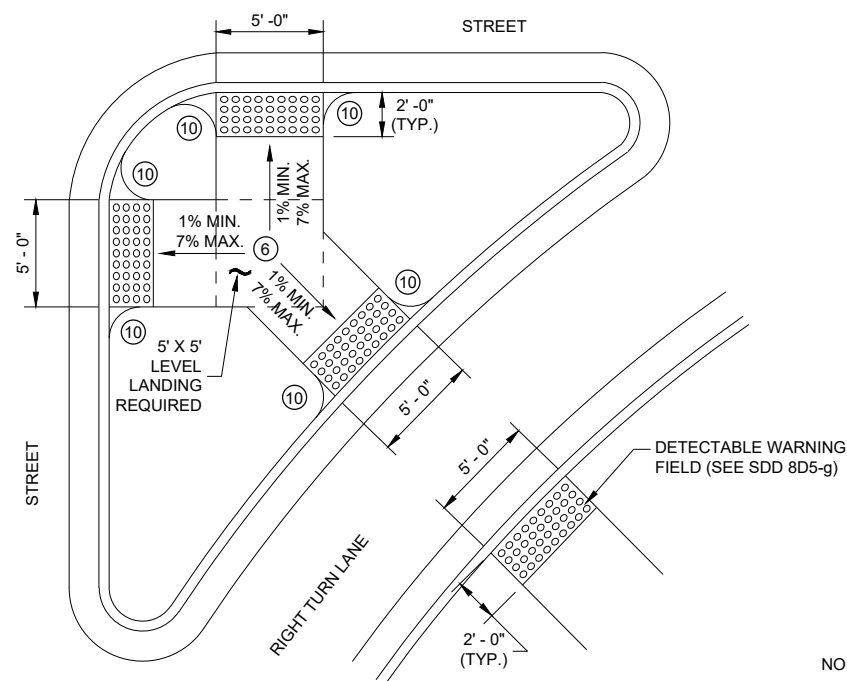
**ISOMETRIC VIEW FOR TYPE 4B1**

**CURB RAMPS TYPE 4B AND 4B1**

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

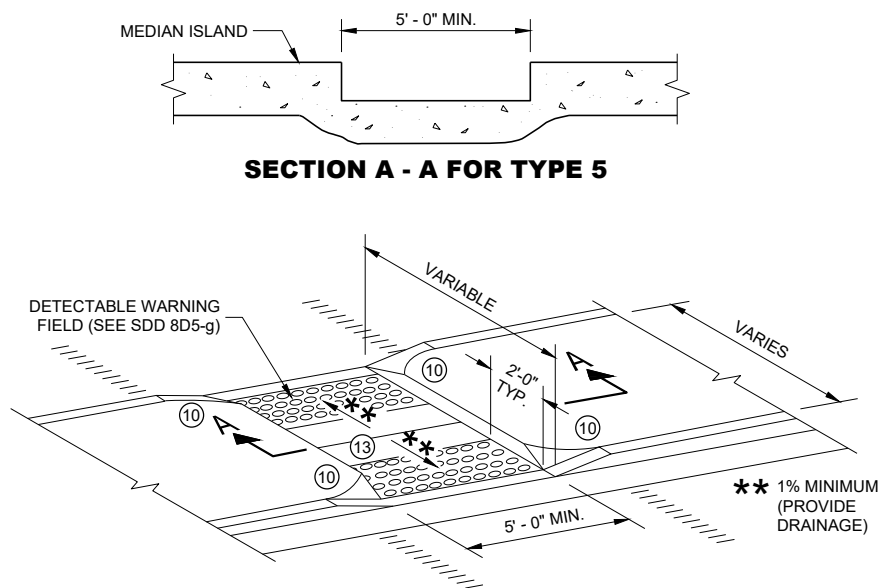


**CURB RAMP TYPE 8  
DETECTABLE WARNINGS  
AT RAILROAD CROSSING**

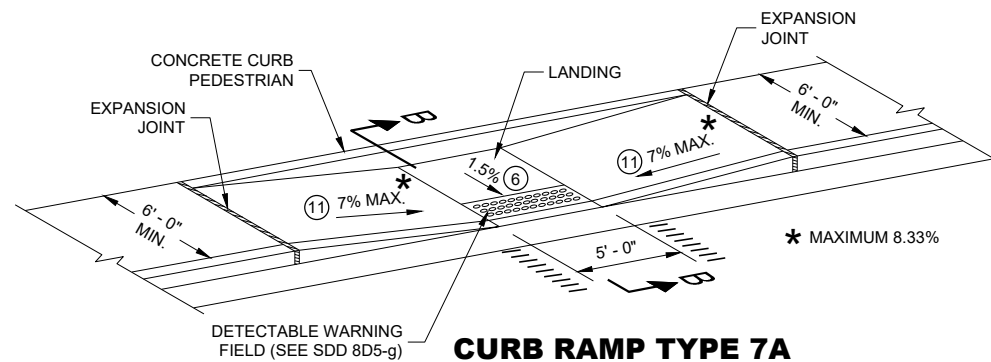


**CURB RAMP TYPE 6  
DETECTABLE WARNING AT ISLANDS**

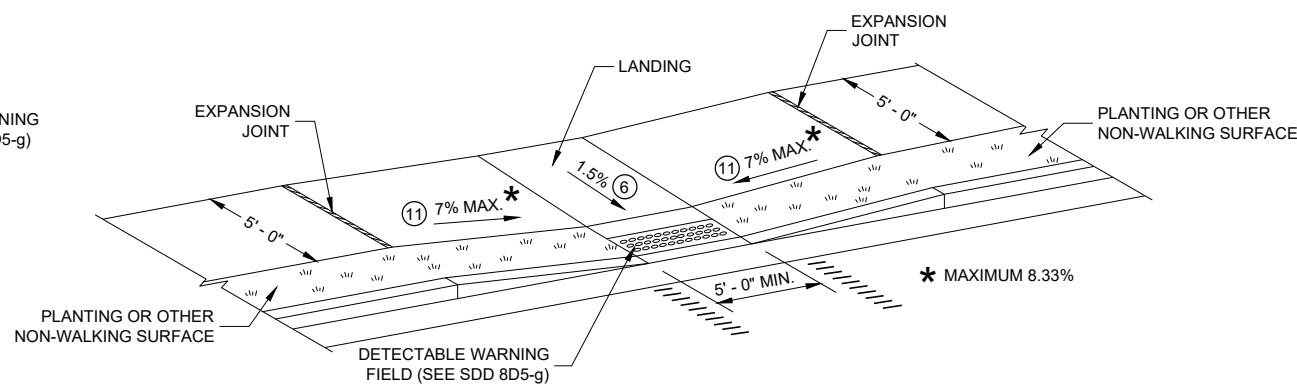
REFER TO GENERAL NOTES (2) AND (3) FOR ALL ISLAND CURB RAMPS



**CURB RAMP TYPE 5  
MEDIAN ISLAND  
NON-ELEVATED PEDESTRIAN CROSSING**



**CURB RAMP TYPE 7A  
MID BLOCK CROSSING**



**CURB RAMP TYPE 7B  
MID BLOCK CROSSING**

NOTE: THESE PARALLEL AND PARALLEL/PERPENDICULAR CURB RAMPS MAY BE USED AT INTERSECTIONS AND MID BLOCK LOCATIONS.

**GENERAL NOTES**

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

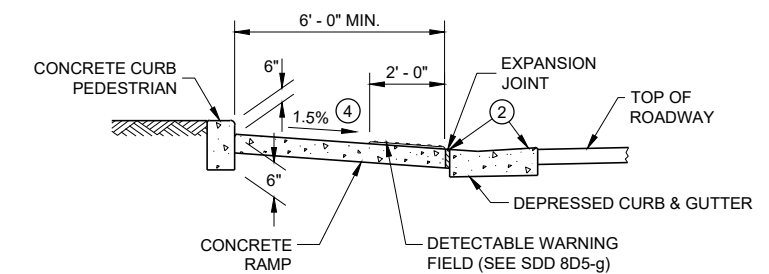
SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

- (2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- (3) AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- (4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- (6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- (10) INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.
- (11) SLOPE SIDEWALK TOWARD LANDING AS SHOWN WHERE THERE IS NO TERRACE OR WHERE THE TERRACE WIDTH IS LESS THAN 6 FEET WIDE.
- (12) THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO A RAILROAD CROSSING SHALL BE 1.5 FEET ±0.1' FROM THE FACE OF THE GATE ARM IF THE GATE ARM EXTENDS ACROSS THE SIDEWALK. WHERE THERE IS NO PEDESTRIAN GATE, THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO THE RAILROAD CROSSING SHALL BE 15 FEET FROM THE NEAREST RAIL.
- (13) DO NOT INSTALL DETECTABLE WARNING FIELDS AT THE EDGES OF STREET-LEVEL PEDESTRIAN REFUGE ISLANDS IF A MINIMUM 2 FOOT CONCRETE SURFACE WITHOUT DETECTABLE WARNINGS (MEASURED IN THE DIRECTION OF PEDESTRIAN TRAVEL) CANNOT BE ACHIEVED.

**LEGEND**

- ===== 1/2" EXPANSION JOINT SIDEWALK
- - - - - CONTRACTION JOINT FIELD LOCATED
- ||||||| PAVEMENT MARKING CROSSWALK (WHITE)

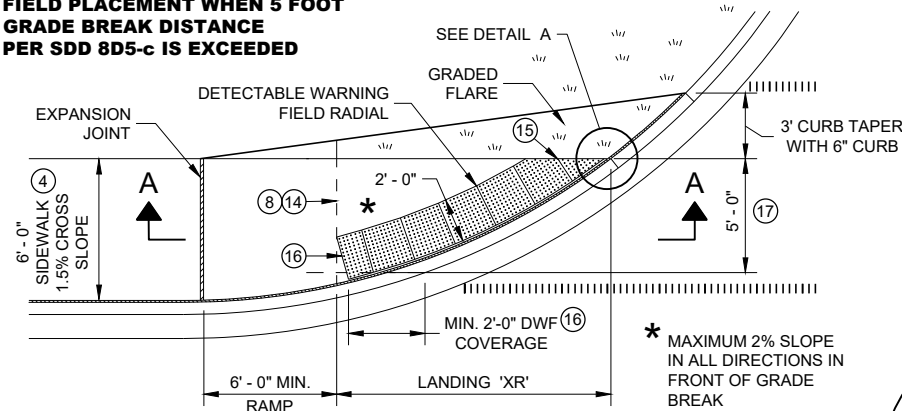


**SECTION B - B FOR TYPE 7A**

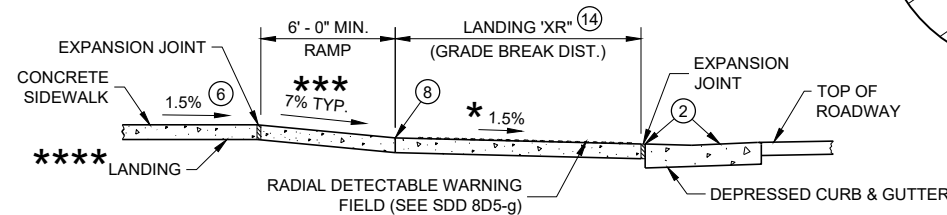
**CURB RAMPS  
TYPE 5, 6, 7A, 7B & 8**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

**RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-c IS EXCEEDED**



**PLAN VIEW CURB RAMP TYPE 4A1 (GRADE BREAK DISTANCE GREATER THAN 5 FEET)**

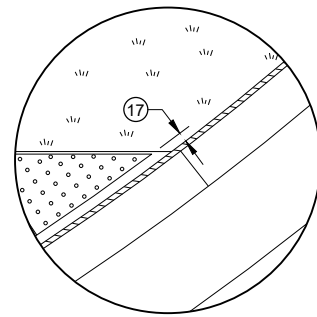


**SECTION A - A FOR TYPE 4A1**

\*\*\*\* IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

\*\*\* MAXIMUM 8.33%

- LEGEND**
- 1/2" EXPANSION JOINT SIDEWALK
  - - - - - CONTRACTION JOINT SIDEWALK
  - ||||| PAVEMENT MARKING CROSSWALK (WHITE)

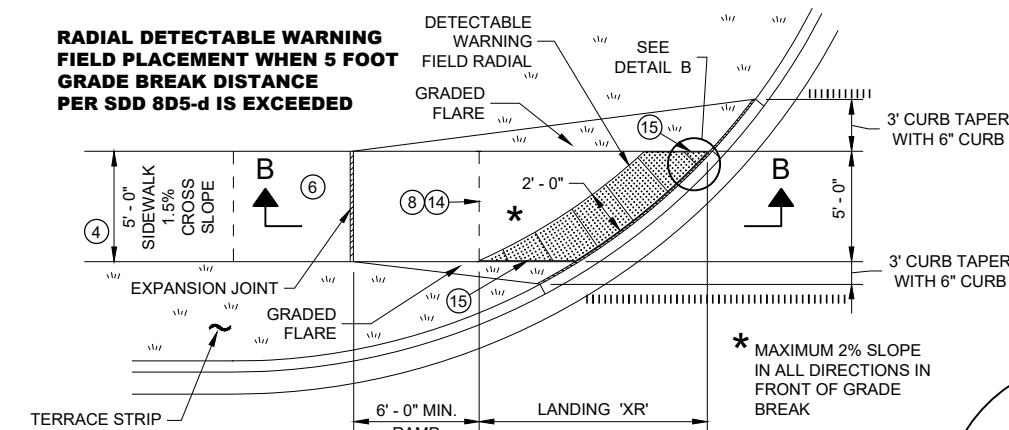


**DETAIL A**

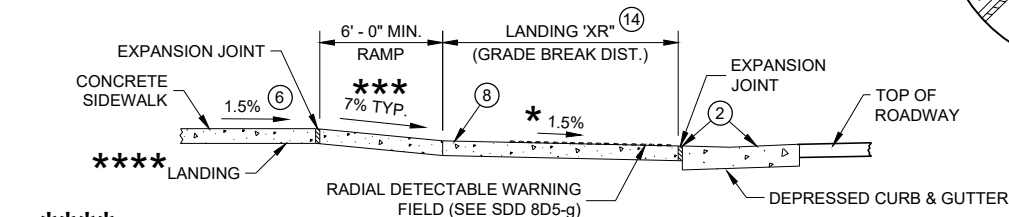
**GENERAL NOTES**

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- APPLY RADIAL DETECTABLE WARNING PLACEMENT SIMILARLY FOR TYPE 4A AND 4A1 CURB RAMPS AND SIMILARLY FOR TYPE 4B AND 4B1 CURB RAMPS. TYPE 4A AND 4B RAMPS ARE NOT SHOWN.
- REFER TO SDD 8D5-g FOR ADDITIONAL RADIAL PLATE REQUIREMENTS.
- FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FILED ARE PROHIBITED.
- DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.
- 2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- 3) AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- 4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- 6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET BY 5 FEET.
- 8) PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- 14) CONSULT ENGINEER IF GRADE BREAK LOCATION (END OF LANDING DIMENSION "XR") REQUIRES FIELD ADJUSTMENT WHEN ESTABLISHING FINAL RADIAL DETECTABLE WARNING FIELD LOCATION.
- 15) FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN 1/8" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.
- 16) USE 1' X 2" RECTANGULAR END PLATE AT END OF TYPE 4A1 RAMP AND PROVIDE MINIMUM 2' - 0" DETECTABLE WARNING FIELD COVERAGE (IN DIRECTION OF PEDESTRIAN TRAVEL) ALONG THE ENTIRE CURB RAMP WIDTH.
- 17) A MAXIMUM 3 INCH CONCRETE BORDER WITH IS ALLOWABLE IN FROM OF RADIAL DETECTABLE WARNING FIELD FOR CONSTRUCTABILITY PURPOSES. CONCRETE BORDER WIDTH MAY VARY UP TO 1 INCH.

**RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-d IS EXCEEDED**



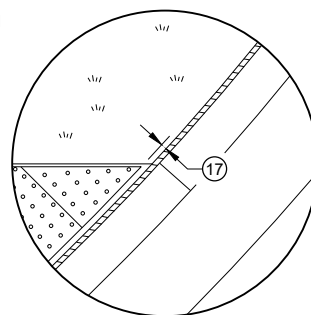
**PLAN VIEW CURB RAMP TYPE 4B1 (GRADE BREAK DISTANCE GREATER THAN 5 FEET)**



**SECTION B - B FOR TYPE 4B1**

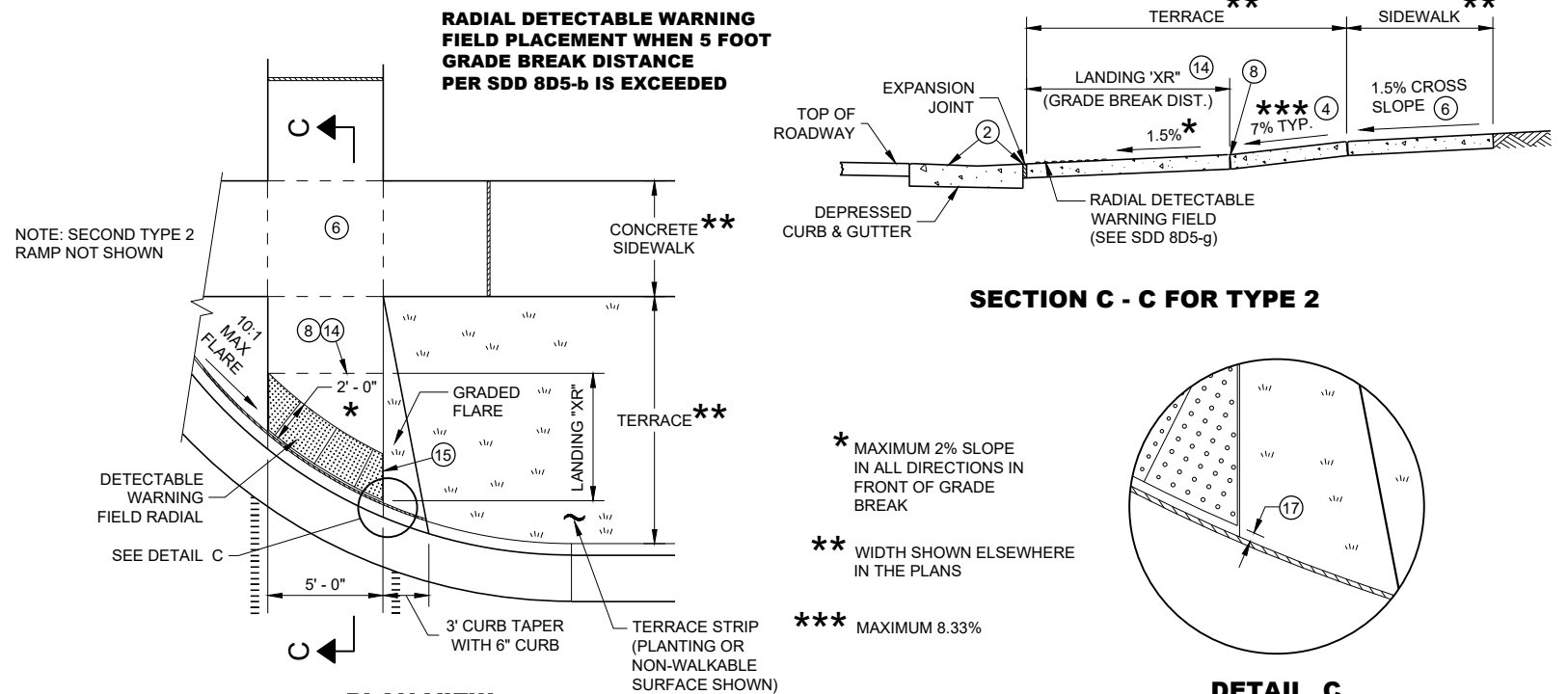
\*\*\*\* IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

\*\*\* MAXIMUM 8.33%



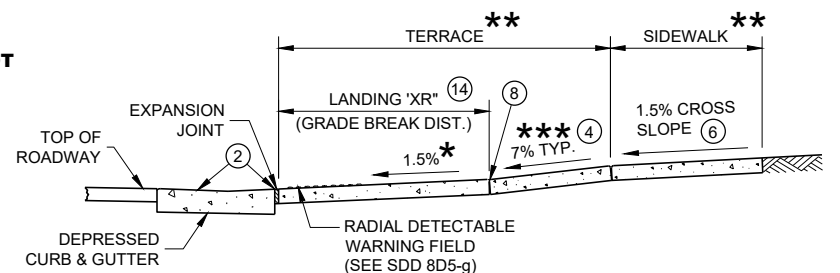
**DETAIL B**

**RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-b IS EXCEEDED**



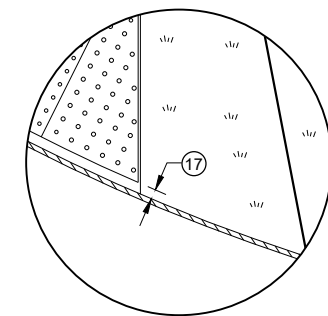
**PLAN VIEW CURB RAMP TYPE 2 (GRADE BREAK DISTANCE GREATER THAN 5 FEET) (ON LINE WITH SIDEWALK)**

NOTE: SECOND TYPE 2 RAMP NOT SHOWN



**SECTION C - C FOR TYPE 2**

- \* MAXIMUM 2% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK
- \*\* WIDTH SHOWN ELSEWHERE IN THE PLANS
- \*\*\* MAXIMUM 8.33%



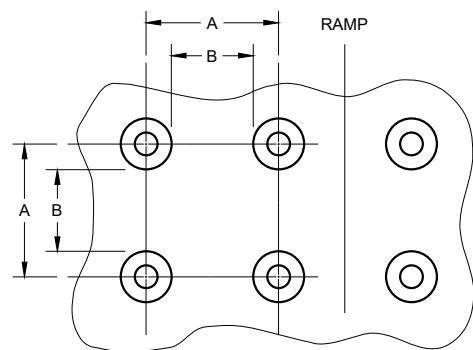
**DETAIL C**

**CURB RAMPS RADIAL DETECTABLE WARNING FIELD APPLICATIONS**

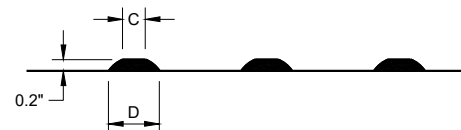
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

	MIN.	MAX.
A	1.6"	2.4"
B	0.65"	1.5"
C	*	*
D	0.9"	1.4"

\* THE C DIMENSION IS 50% TO 65% OF THE D DIMENSION.

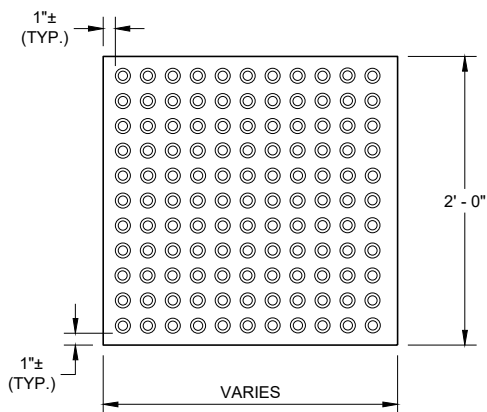


**PLAN VIEW**

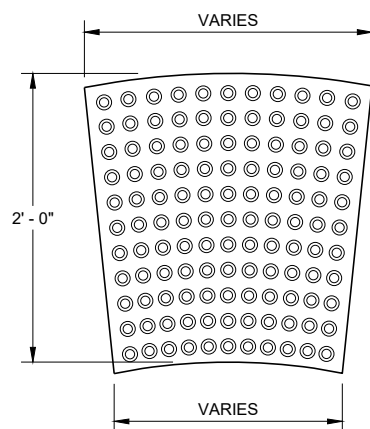


**ELEVATION VIEW**

**TRUNCATED DOMES  
DETECTABLE WARNING PATTERN DETAIL**

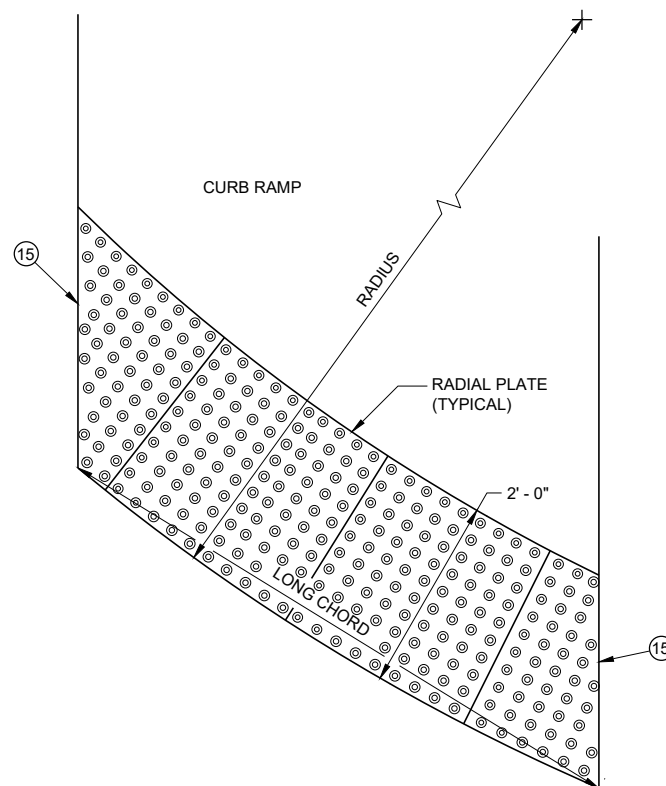


**RECTANGULAR  
PLATES**

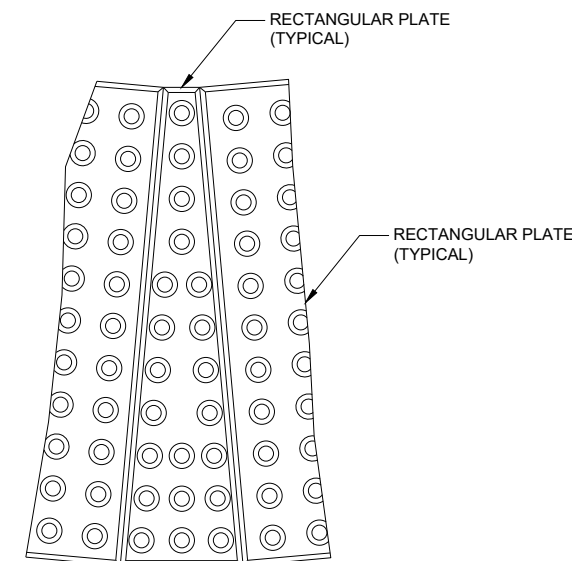


**RADIAL  
PLATES**

**PLAN VIEW  
DETECTABLE WARNING FIELDS (TYPICAL)**



**PLAN VIEW  
RADIAL DETECTABLE  
WARNING FIELD ATTRIBUTES**



**PLAN VIEW  
RADIAL WEDGE PLATE  
CONNECTION DETAIL**

**GENERAL NOTES**

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AT A CURB RAMP SHALL BE FROM THE SAME MANUFACTURER.

PLACE ALL DETECTABLE WARNING FIELD SYSTEMS IN ACCORDANCE TO THE MANUFACTURER'S RECOMMENDATION.

FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FIELD ARE PROHIBITED.

DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.

FOR RADIAL DETECTABLE WARNING FIELD APPLICATIONS WHERE STANDARD RADIAL PLATES ARE NOT AVAILABLE AT AN INTERSECTION CURB RADIUS, A COMBINATION OF SQUARE OR RECTANGULAR PLATES AND RADIAL PLATES MAY BE USED TO FORM RADIAL CONFIGURATION. RADIAL WEDGE PLATES IN COMBINATION WITH SQUARE PLATES ARE ALSO ACCEPTABLE. FOLLOW MANUFACTURER'S RECOMMENDATIONS.

REFER TO CONTRACT AND STANDARD SPECIFICATIONS FOR FIELD CUTTING REQUIREMENTS.

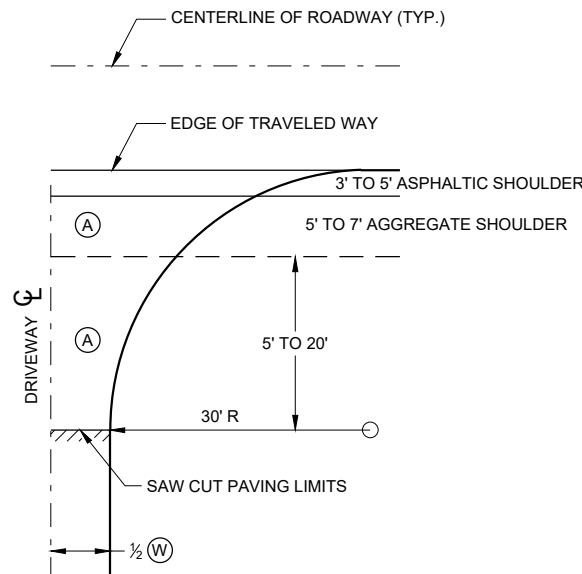
DO NOT EMBED IN CONCRETE ANY FIELD-CUT PLATES WITH CUT EDGES SHORTER THAN 6 INCHES. CONSULT WITH MANUFACTURER FOR RE-DRILLING AND ANCHORING REQUIREMENTS OF FIELD-CUT PLATES.

15 FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN 1/8" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.

<b>CURB RAMPS RECTANGULAR AND RADIAL DETECTABLE WARNING PLATES</b>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2019 DATE	/S/ Rodney Taylor ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR
<small>FHWA</small>	

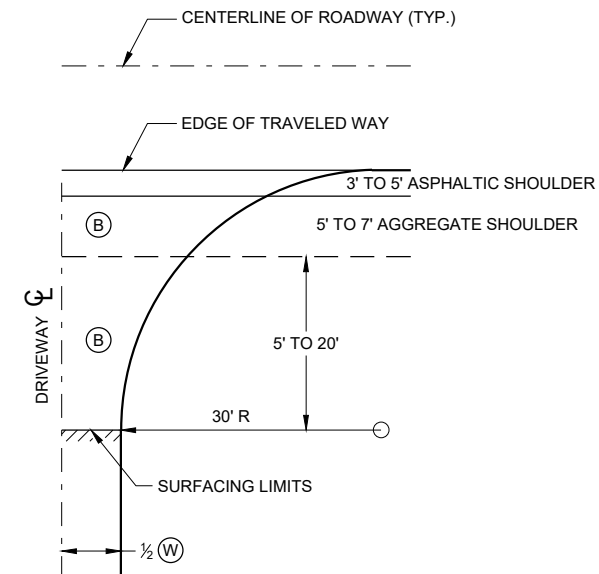
**GENERAL NOTES**

- ① DESIGN WILL DETERMINE FINAL DRIVEWAY ASPHALTIC THICKNESS BASED ON TYPE OF USAGE AND LOADINGS.

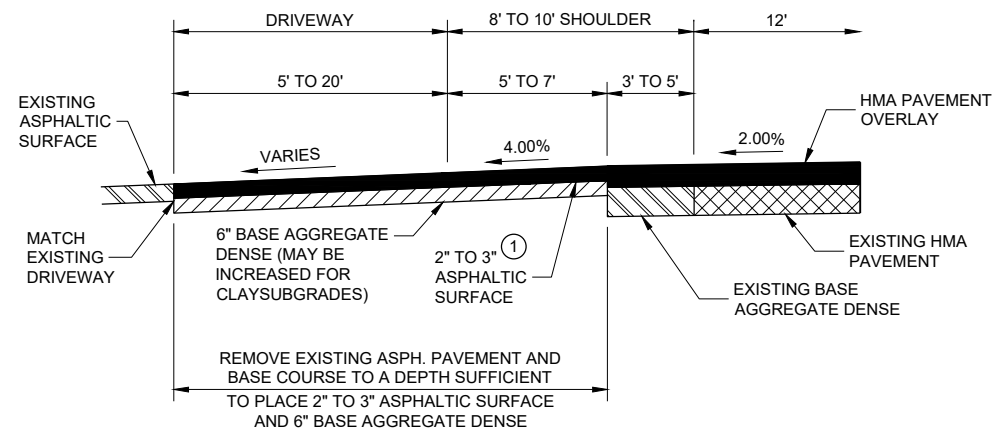


- Ⓐ : PAID FOR AS ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES. (TON)
- Ⓑ : PAID FOR AS BASE AGGREGATE DENSE 1 1/4" (TON)
- ⒲ : DRIVEWAY WIDTH 16' MIN. - 24' MAX.

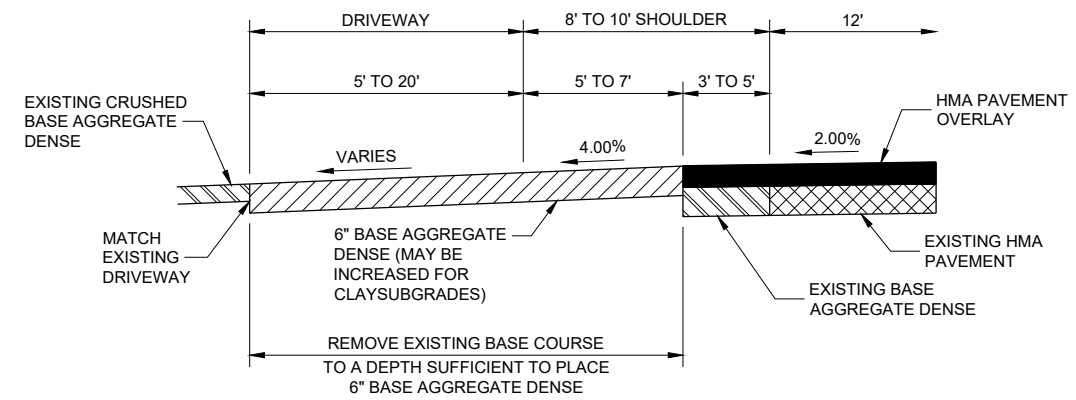
**PLAN VIEW  
HALF SECTION**



**PLAN VIEW  
HALF SECTION**



**PROFILE VIEW  
RURAL ENTRANCE  
WITH ASPHALTIC SURFACE  
RESURFACING PROJECTS**



**PROFILE VIEW  
RURAL ENTRANCE  
WITH AGGREGATE SURFACE  
6" BASE AGGREGATE DENSE  
RESURFACING PROJECTS**

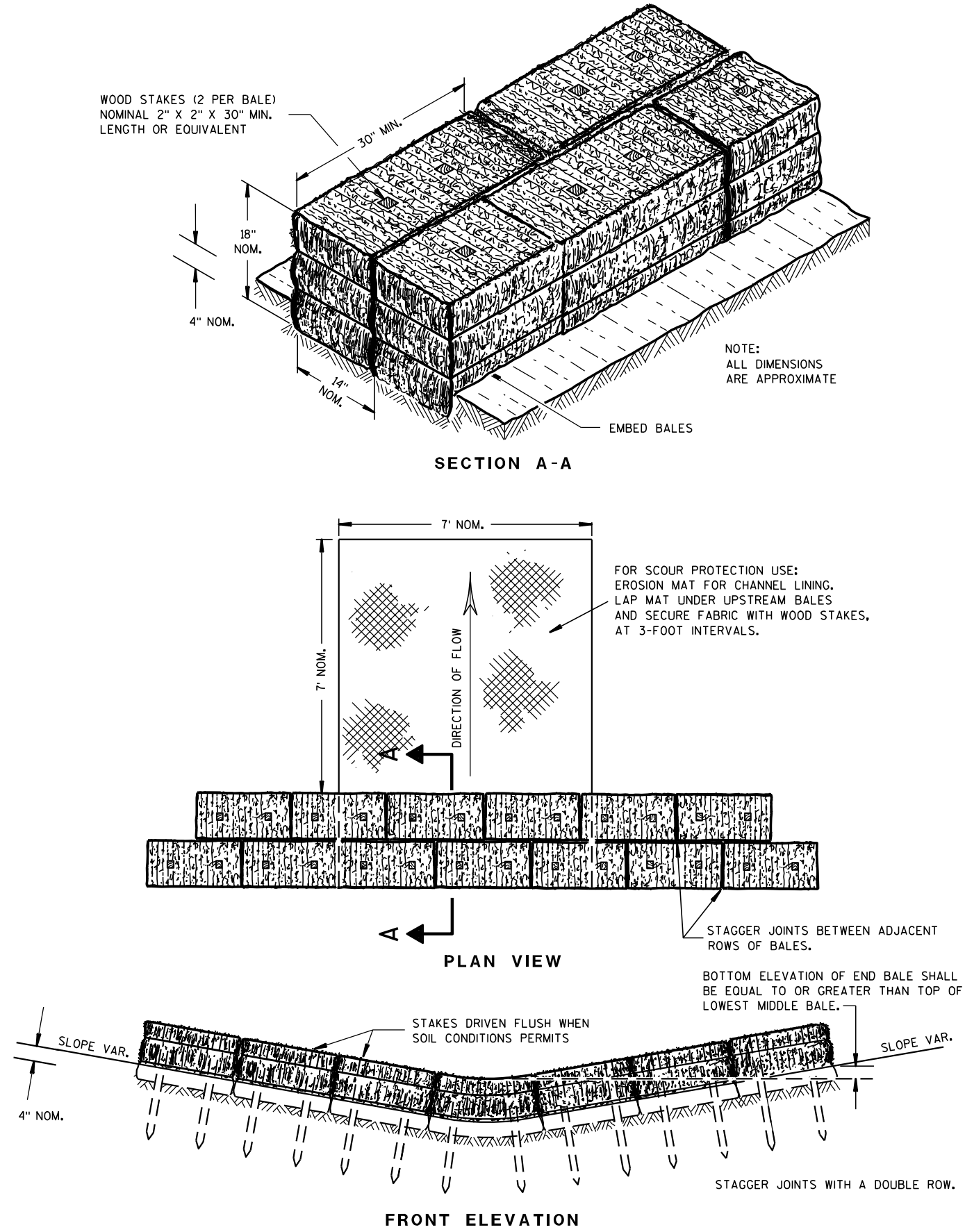
**DRIVEWAYS WITHOUT CURB  
AND GUTTER RESURFACING  
PROJECTS RURAL**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
December 2016 /S/ Rodney Taylor  
DATE ROADWAY STANDARDS DEVELOPMENT  
ENGINEER

FHWA



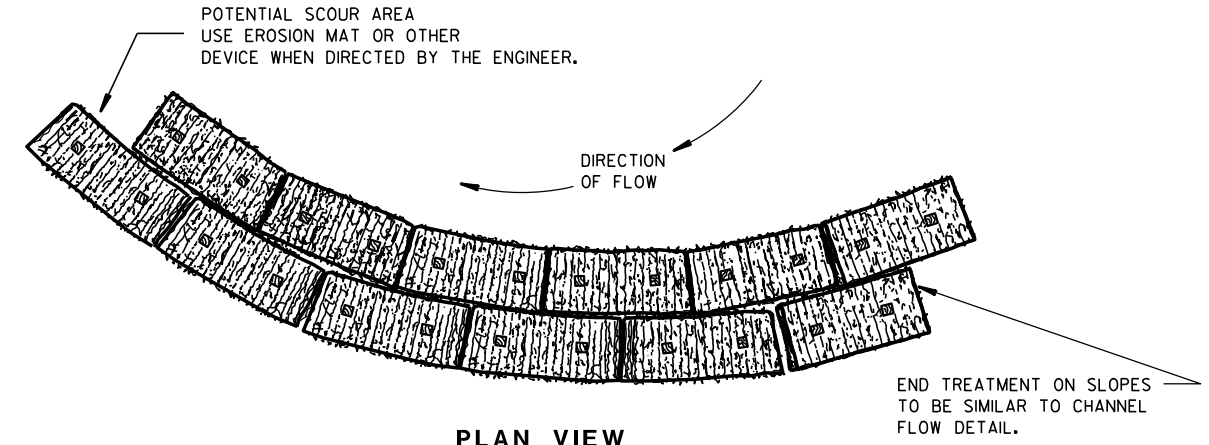


TEMPORARY DITCH CHECK USING EROSION BALES ①

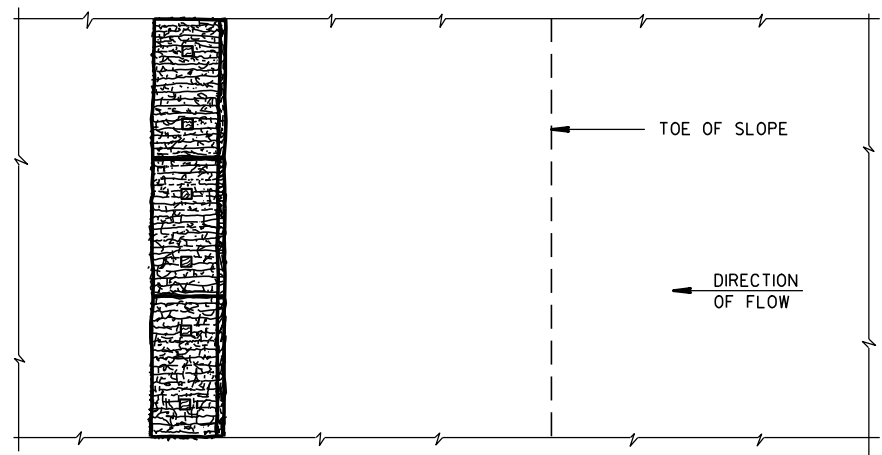
GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

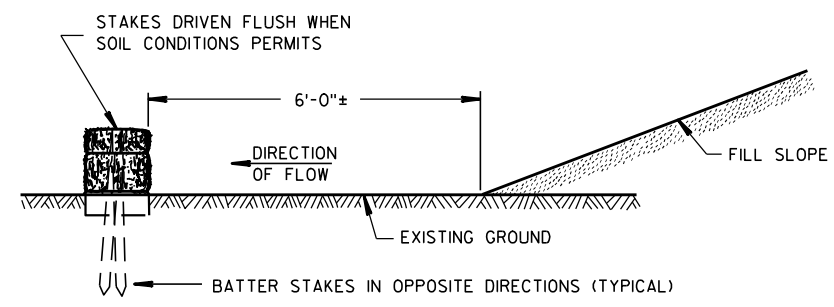
- ① TEMPORARY DITCH CHECKS EITHER EROSION BALES OR MANUFACTURED SHALL BE PAID FOR UNDER THE BID ITEM OF TEMPORARY DITCH CHECK. THE DEPARTMENT WILL NOT PAY FOR TEMPORARY DITCH CHECKS CONSTRUCTED OF A SINGLE ROW OF EROSION BALES.



PLAN VIEW WHEN ALTERING THE DIRECTION OF FLOW



PLAN VIEW

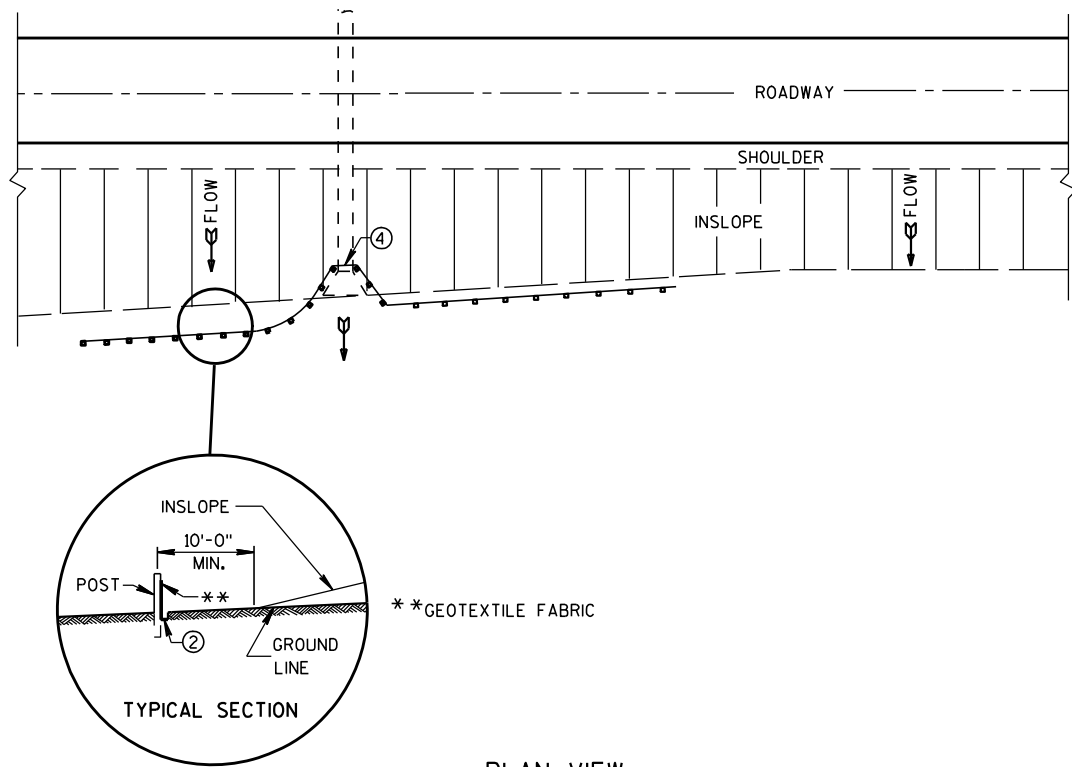


FRONT ELEVATION WHEN EXISTING GROUND SLOPES AWAY FROM FILL SLOPE EROSION BALES FOR SHEET FLOW

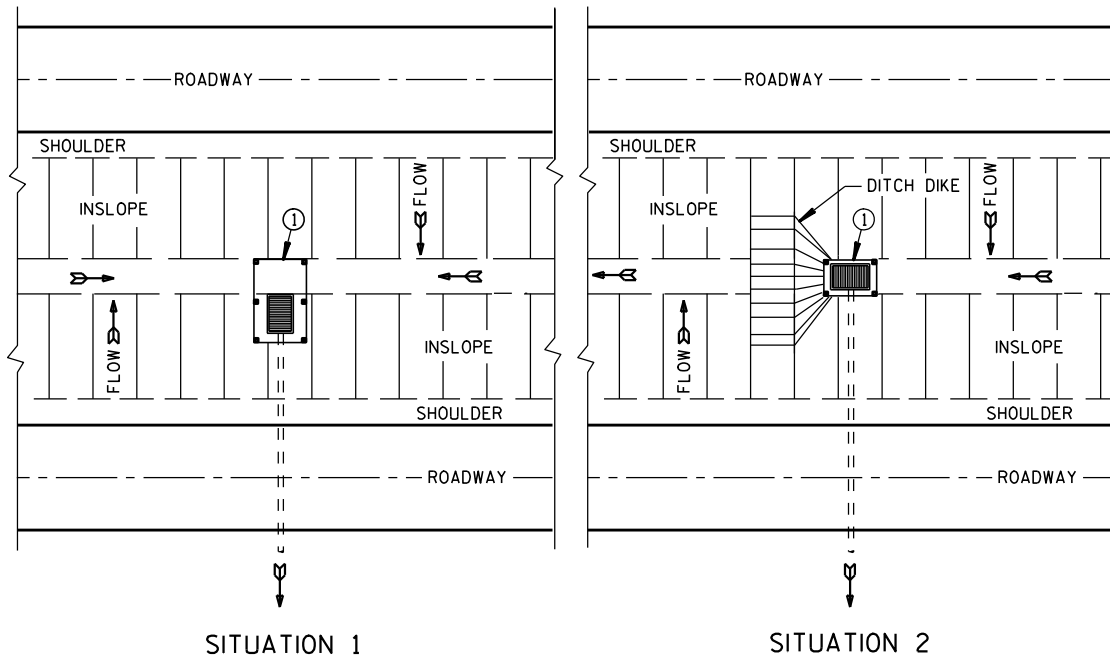
TYPICAL INSTALLATIONS OF EROSION BALES / TEMPORARY DITCH CHECKS

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED  
 6/04/02 /S/ Beth Canestra  
 DATE CHIEF ROADWAY DEVELOPMENT ENGINEER  
 FHWA



PLAN VIEW  
TYPICAL APPLICATION OF SILT FENCE

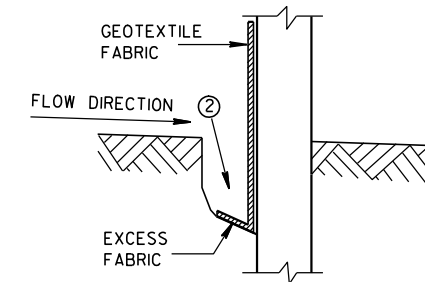


SITUATION 1 SITUATION 2  
PLAN VIEW  
SILT FENCE AT MEDIAN SURFACE DRAINS

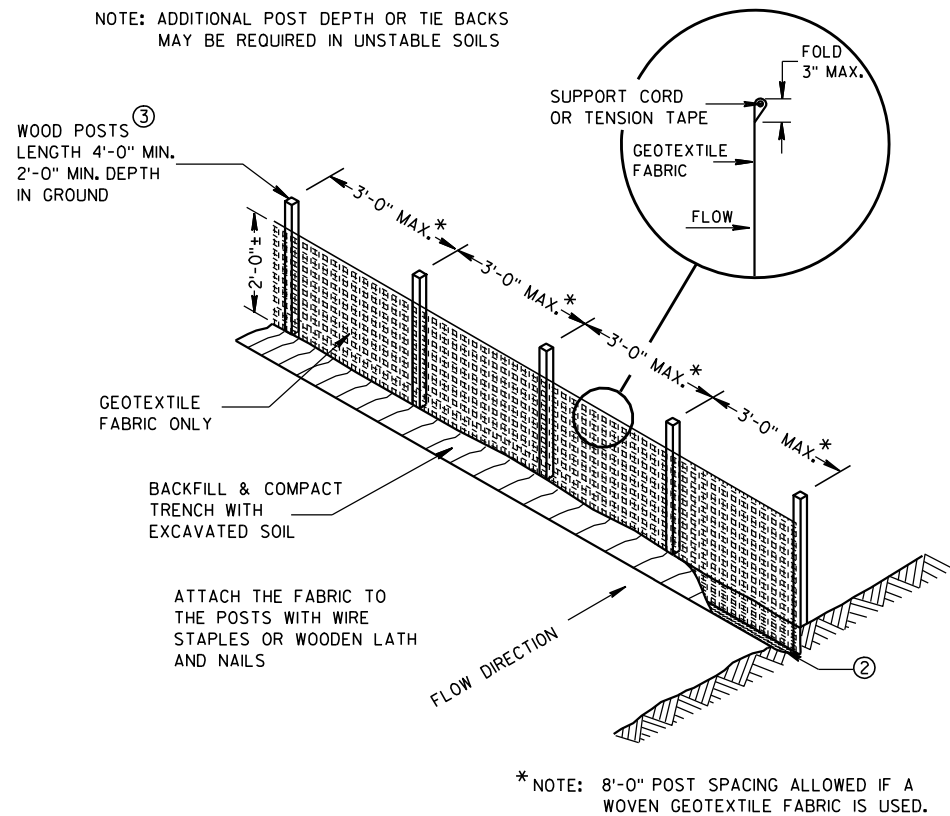
**GENERAL NOTES**

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

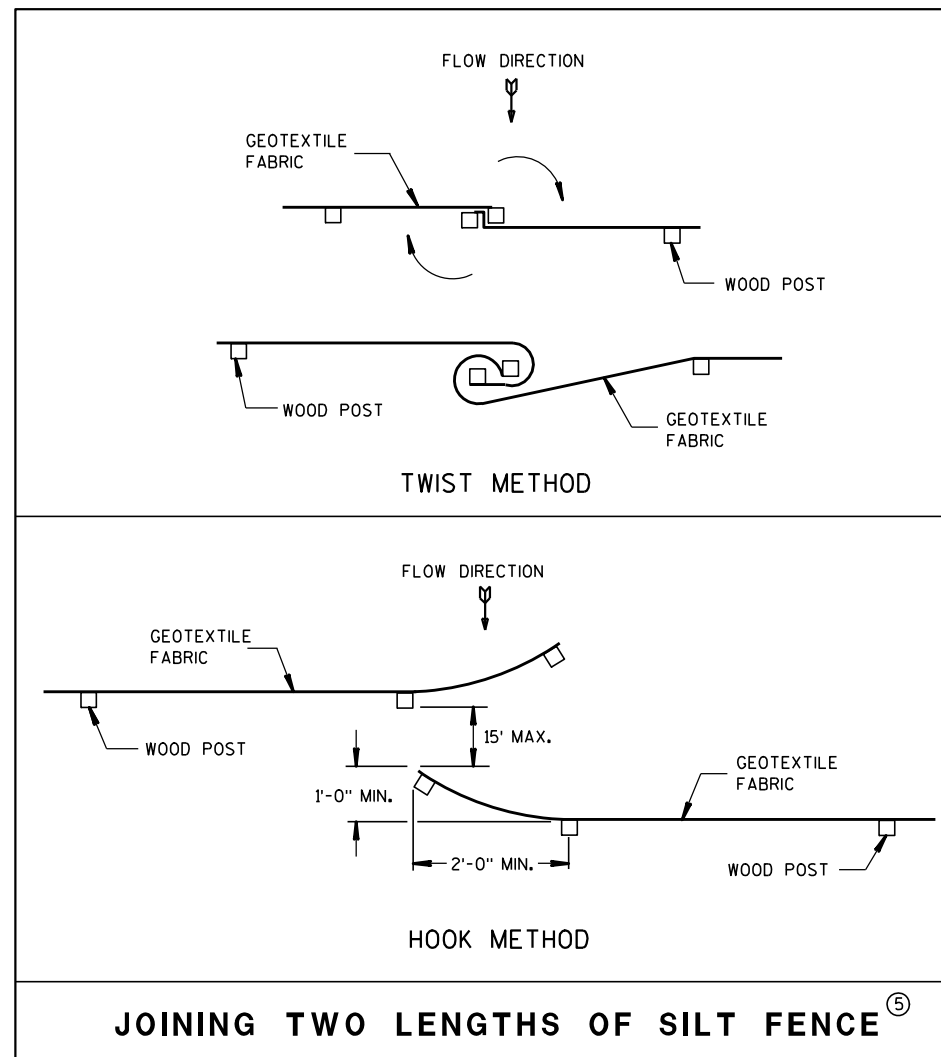
- ① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- ③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1 1/8" X 1 1/8" OF OAK OR HICKORY.
- ④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- ⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



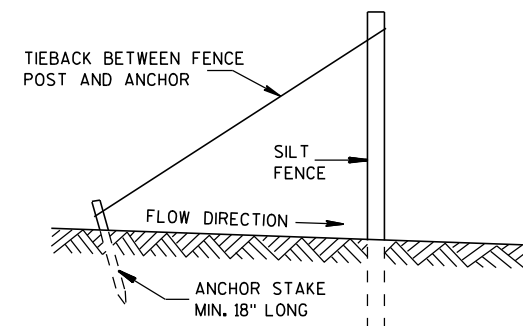
TRENCH DETAIL



SILT FENCE



JOINING TWO LENGTHS OF SILT FENCE ⑤

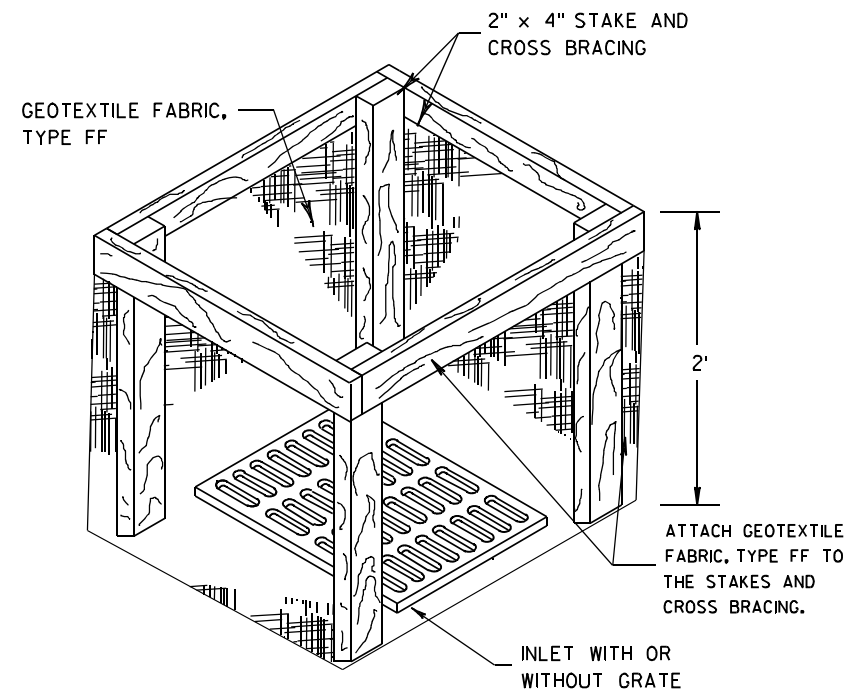
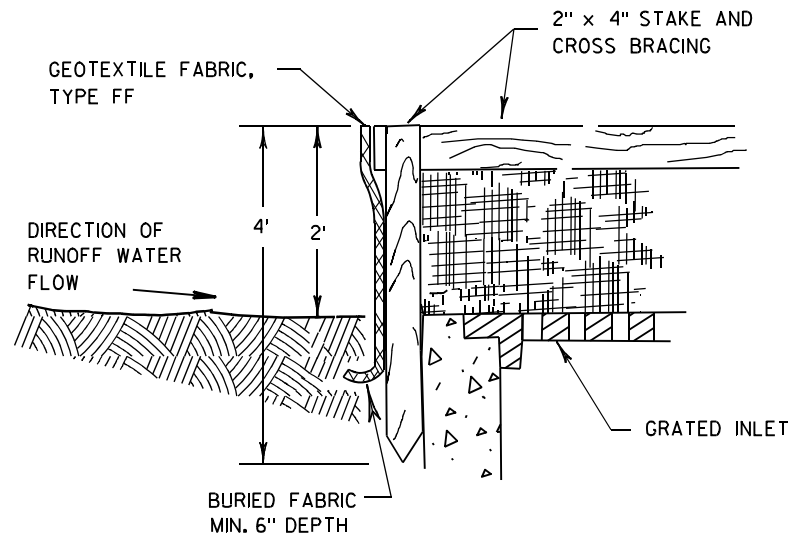


SILT FENCE TIE BACK  
(WHEN REQUIRED BY THE ENGINEER)

**SILT FENCE**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
4-29-05 /S/ Beth Canestra  
DATE CHIEF ROADWAY DEVELOPMENT ENGINEER  
FHWA



**INLET PROTECTION, TYPE A**

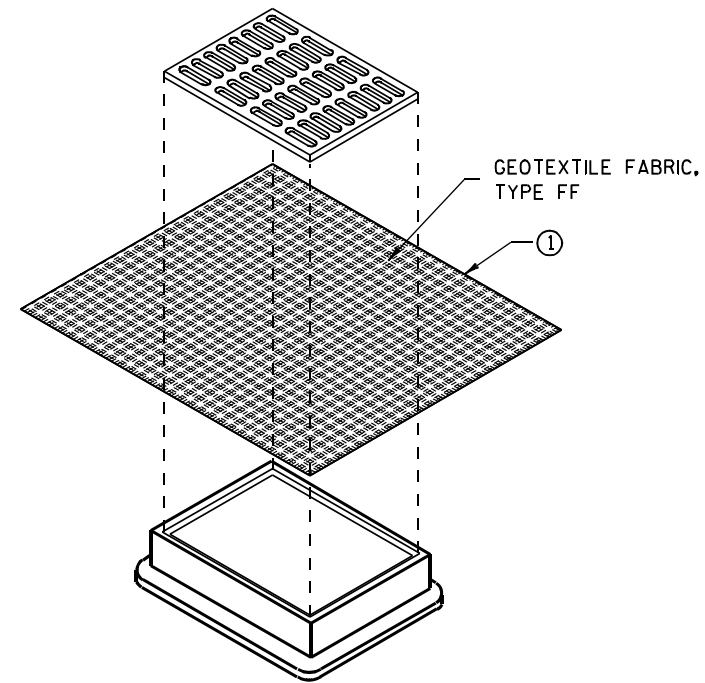
**GENERAL NOTES**

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

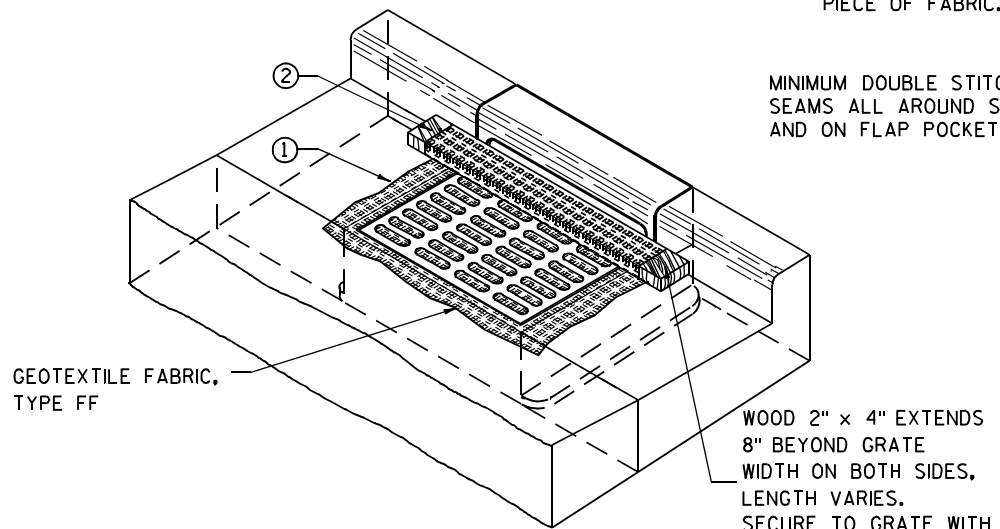
WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- ① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- ② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- ③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



**INLET PROTECTION, TYPE B  
(WITHOUT CURB BOX)**

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



**INLET PROTECTION, TYPE C (WITH CURB BOX)**

**INSTALLATION NOTES**

**TYPE B & C**

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

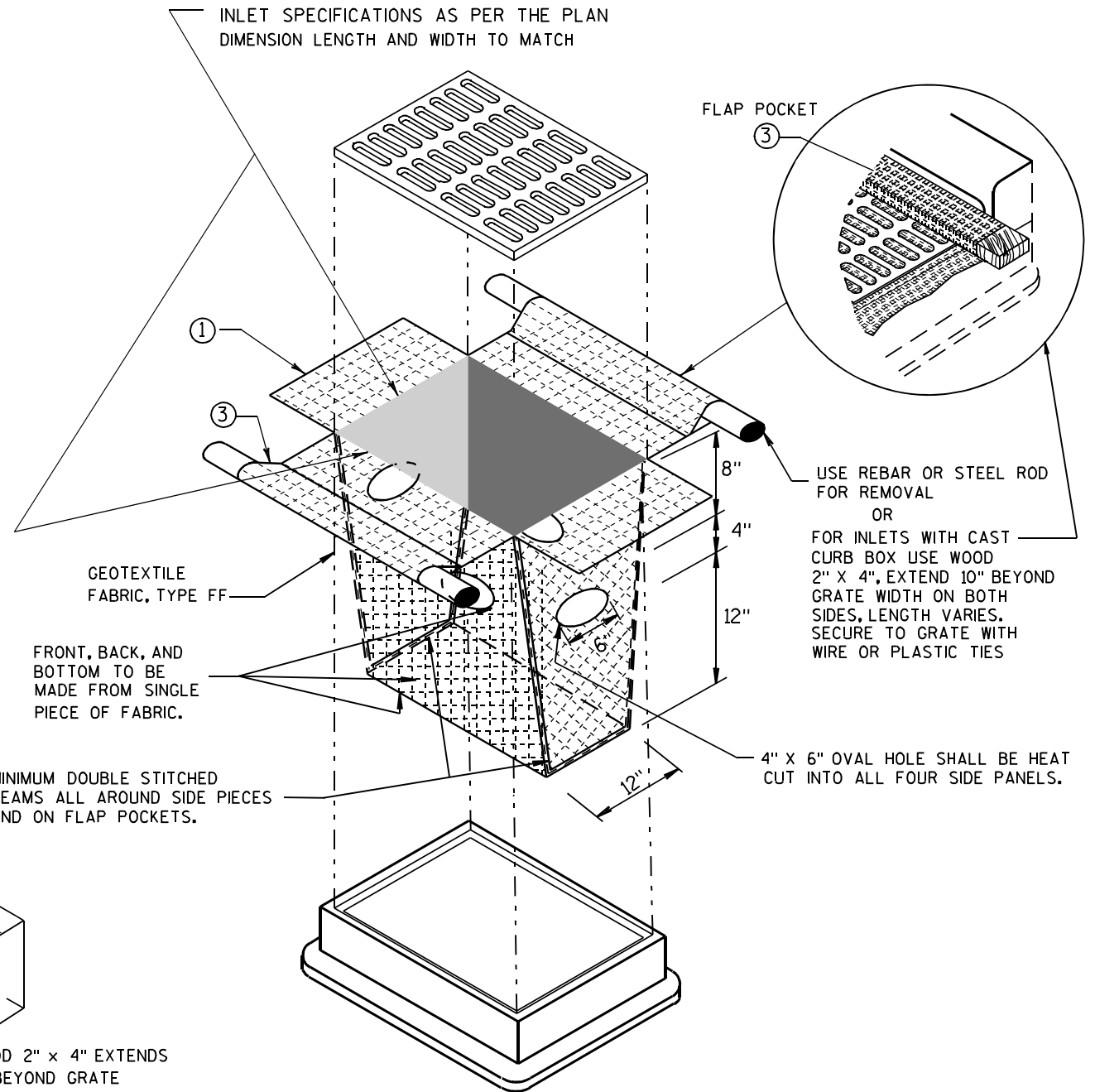
THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

**TYPE D**

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

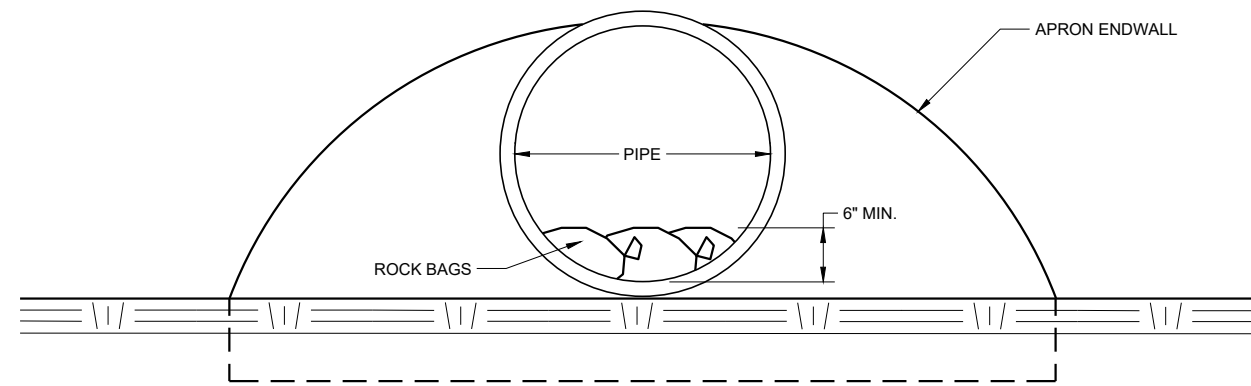
THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.



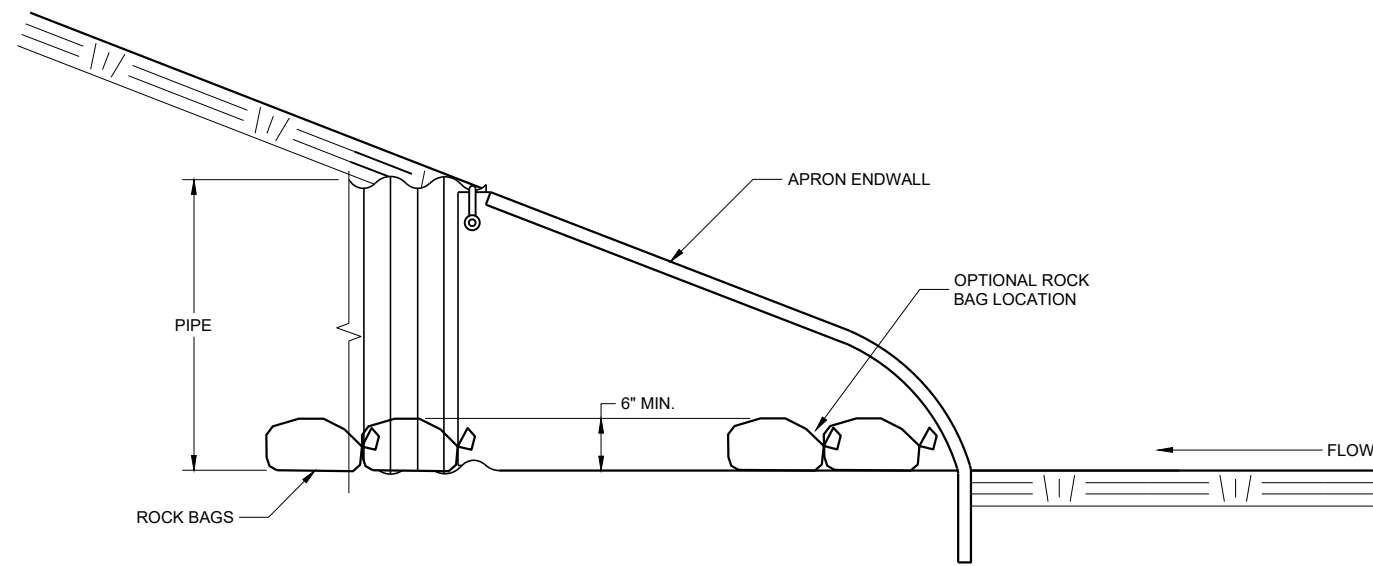
**INLET PROTECTION, TYPE D**

(CAN BE INSTALLED IN ANY INLET TYPE WITH OR WITHOUT A CURB BOX AS PER NOTE ②)

<b>INLET PROTECTION TYPE A, B, C, AND D</b>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 10/16/02 DATE	/s/ Beth Connestra CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA	



**END VIEW**



**SIDE VIEW**

**CULVERT PIPE CHECK**  
(INSTALL ON INLET END ONLY)

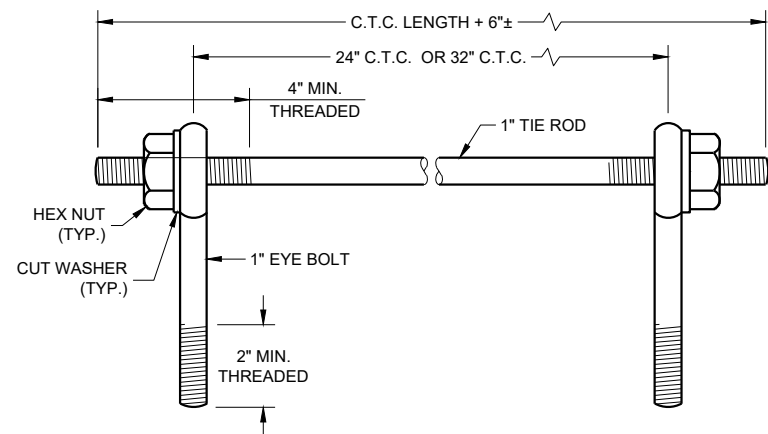
**CULVERT PIPE CHECK**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

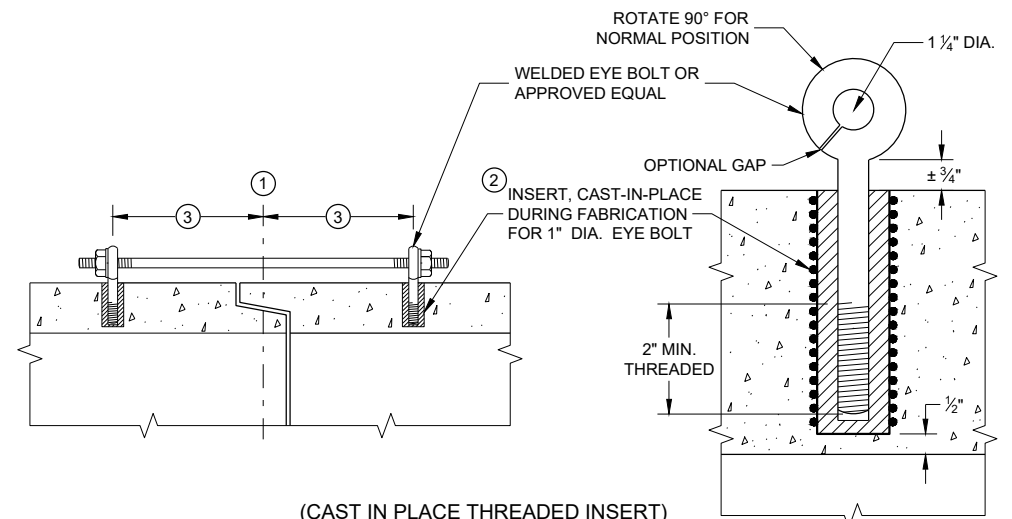
APPROVED  
May 2019 /S/ Daniel Schave  
DATE EROSION CONTROL ENGINEER

FHWA





**EYE BOLTS AND TIE ROD**  
**EYE BOLT AND TIE ROD ASSEMBLY (ALTERNATE NO. 1)**



(CAST IN PLACE THREADED INSERT)  
**LONGITUDINAL SECTIONS**

**GENERAL NOTES**

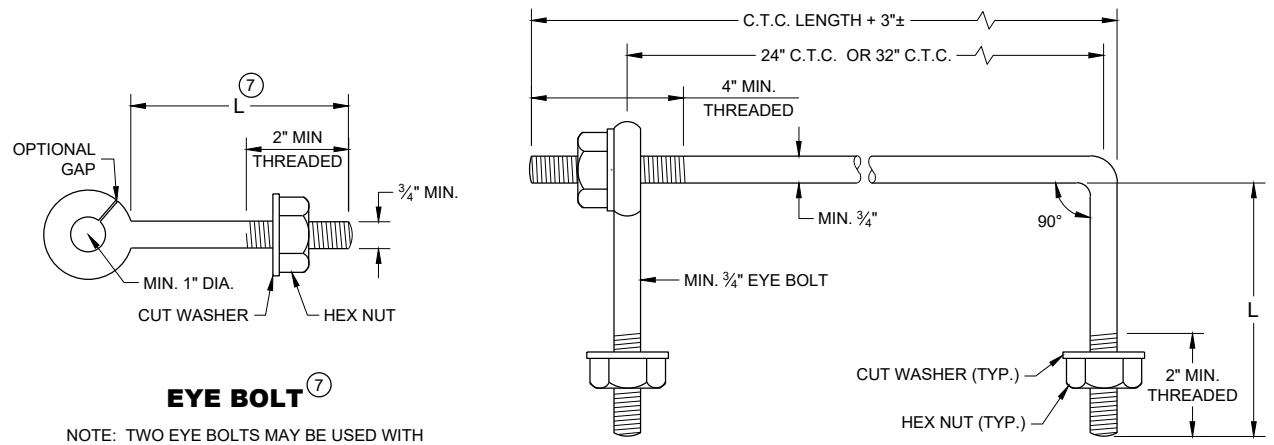
DETAILS OF CONSTRUCTION, MATERIALS, AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

CONCRETE CULVERT AND STORM SEWER PIPE SHALL BE TIED TOGETHER IN THE MANNER ILLUSTRATED BY THIS DETAIL AT LOCATIONS DESIGNATED IN THE STANDARD SPECIFICATIONS AND THE PLAN. THE CONTRACTOR MAY USE EITHER ALTERNATE 1, 2 OR 3 FOR DRAINAGE STRUCTURES. ONLY ALTERNATE 1 AND 3 MAY BE USED FOR CATTLE PASSES, UNLESS OTHERWISE STATED IN THE CONTRACT. THE MATERIALS, FABRICATION AND WORK NECESSARY TO TIE THE PIPE BY THIS DETAIL WILL BE CONSIDERED INCIDENTAL TO THE PIPE AND APRON ENDWALLS IF REQUIRED.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR JOINT TIES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.

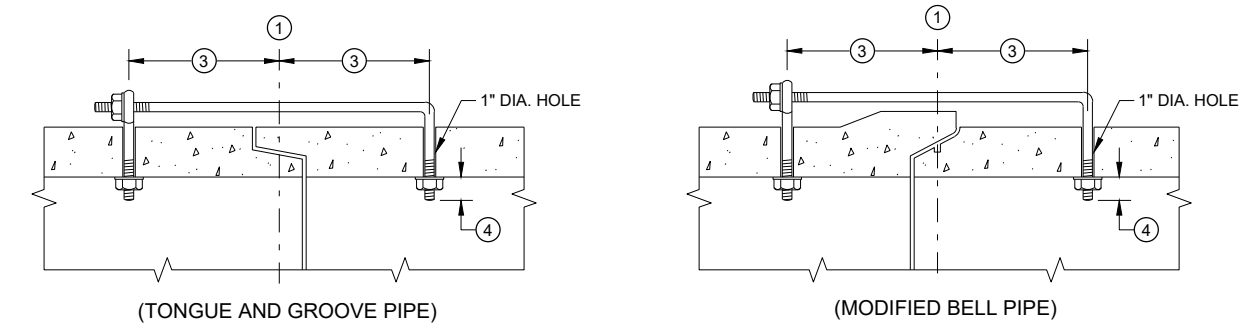
JOINT TIES TO BE HOT-DIP GALVANIZED PER ASTM A 153.

- ① CENTER LINE OF TONGUE AND GROOVE OR BELL AND SPIGOT JOINTS.
- ② THE INSIDE OF THE THREADED INSERTS SHALL BE CLEAN TO ALLOW THE INSERTION OF THREADED EYE BOLTS.
- ③ HOLES SHALL BE CAST-IN-PLACE OR DRILLED PER THE APPLICABLE DETAIL, AND EQUAL DISTANCE FROM THE CENTERLINE OF THE JOINT.
- ④ BOLT PROJECTION INSIDE OF PIPE SHALL NOT EXCEED 2 INCHES.
- ⑤ OPENING TO BE ROD DIAMETER PLUS 1 INCH.
- ⑥ LENGTH ADEQUATE TO EXTEND TO WITHIN 1/2 INCH OF THE INNER SURFACE OF THE PIPE.
- ⑦ EYE BOLT LENGTH DETERMINED BY WALL THICKNESS, BELL THICKNESS AND BOLT PROJECTION INSIDE PIPE.



**EYE BOLT AND TIE ROD**

**EYE BOLT**  
 NOTE: TWO EYE BOLTS MAY BE USED WITH A 30" OR 38" LONG THREADED ROD IN LIEU OF THE 90° BENT TIE ROD.



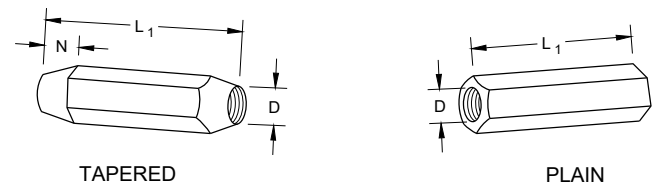
**LONGITUDINAL SECTION**  
 (JOINT TIES FOR 18" TO 66" DIA. CONCRETE PIPE)

**EYE BOLT AND TIE ROD ASSEMBLY (ALTERNATE NO. 2)**

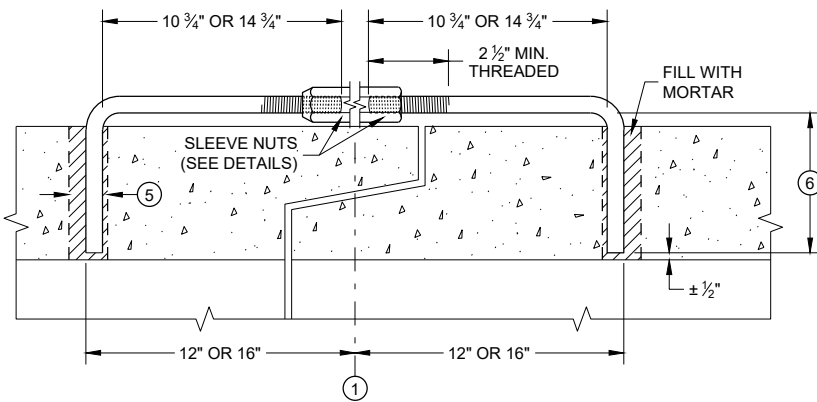
**ADJUSTABLE TIE ROD TABLE**

PIPE DIAMETER	TIE ROD DIAMETER	D	L <sub>1</sub>	N
12 - 60	5/8	5/8	5	1/2
66 - 84	3/4	3/4	5	1/2
90 - 144	1	1	7	1 7/16

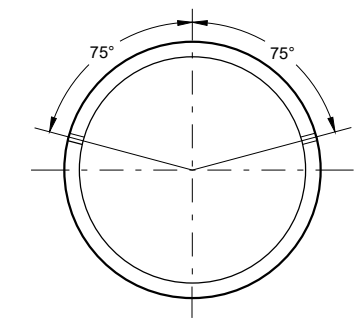
DIMENSIONS SHOWN ARE IN INCHES



**RIGHT AND LEFT THREADS SLEEVE NUTS**

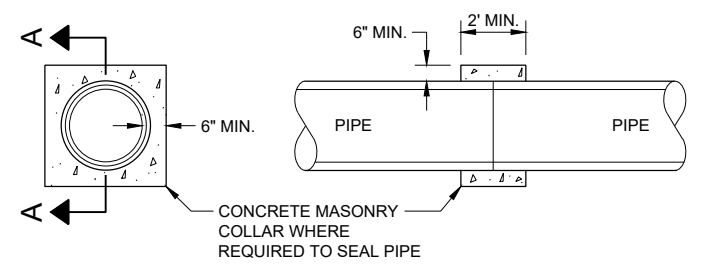


**LONGITUDINAL SECTION**  
**ADJUSTABLE TIE ROD (ALTERNATE NO. 3)**



PLACEMENT OF (2) CAST-IN-PLACE INSERTS OR HOLES DURING FABRICATION FOR PIPE SECTIONS REQUIRING TIE RODS

**TRANSVERSE SECTION**

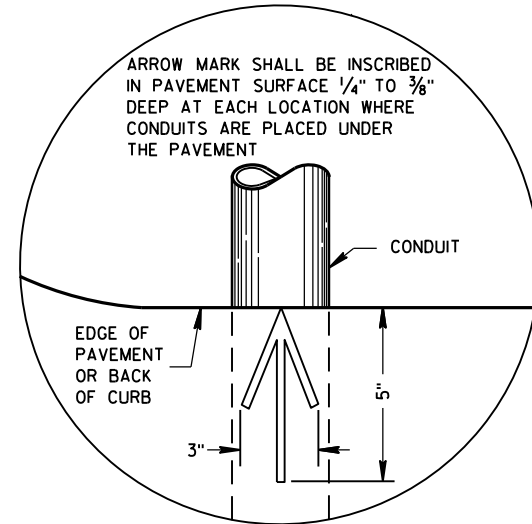


**SECTION A - A**  
**CONCRETE COLLAR DETAIL**

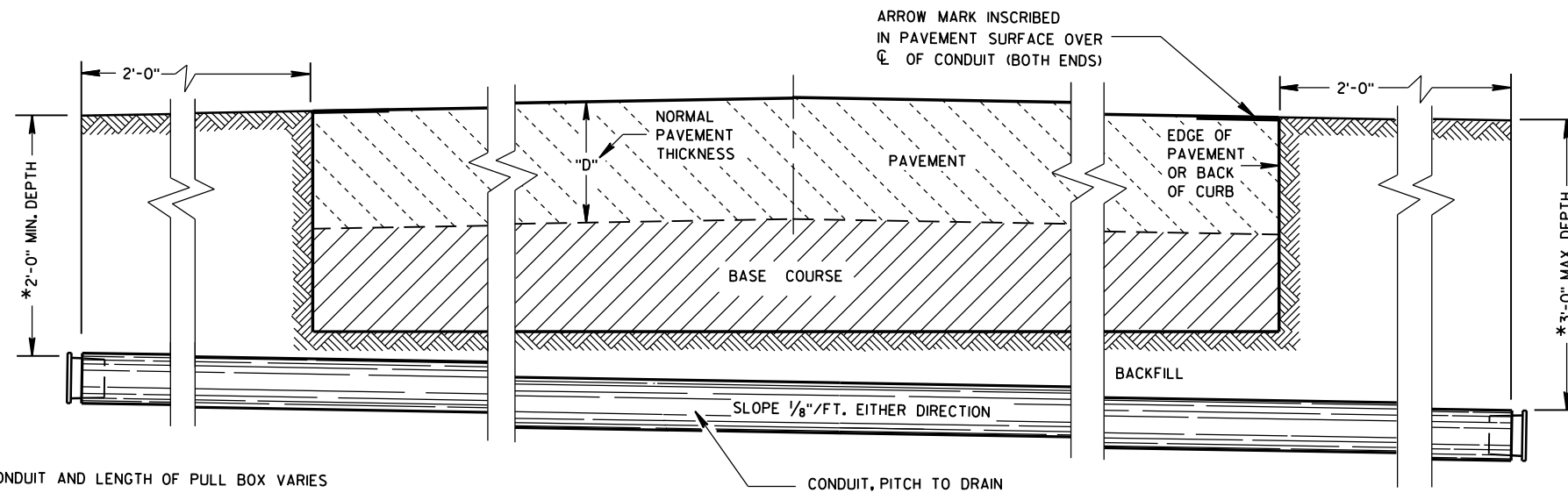
**JOINT TIES FOR CONCRETE PIPE AND CONCRETE COLLAR DETAIL**

STATE OF WISCONSIN  
 DEPARTMENT OF TRANSPORTATION

APPROVED  
 November 2021 /S/ Rodney Taylor  
 DATE ROADWAY STANDARDS DEVELOPMENT  
 ENGINEER



**PLAN VIEW  
ARROW MARK**



**SIDE ELEVATION  
DETAIL FOR CONDUIT UNDER PAVED HIGHWAYS**

\*DEPTH OF CONDUIT AND LENGTH OF PULL BOX VARIES WITH HEIGHT OF CURB USED. ALSO SEE PULL BOX S.D.D. 9B4

**GENERAL NOTES**

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

METALLIC (STANDARD SPECIFICATION 652.2.2) OR NONMETALLIC (STANDARD SPECIFICATION 652.2.3) CONDUIT SHALL BE FURNISHED AND PLACED AS SHOWN.

DEPTH OF CONDUIT INSTALLED BELOW THE TRAVELED WAY SHALL BE 24 INCHES MINIMUM AND 36 INCHES MAXIMUM.

DEPTH OF CONDUIT INSTALLED THAT IS NOT BELOW THE TRAVELED WAY SHALL BE 18 INCHES MINIMUM AND 36 INCHES MAXIMUM.

ANY EXCEPTION TO THE MAXIMUM DEPTH SHALL BE ONLY WITH THE WRITTEN APPROVAL OF THE ENGINEER.

THE TRENCH SHALL NOT BE BACKFILLED PRIOR TO INSPECTION OF THE CONDUIT.

ALL METALLIC CONDUIT RACEWAY ENDS SHALL BE REAMED AND THREADED.

ALL METALLIC CONDUIT IN WHICH WIRE OR CABLE IS TO BE INSTALLED SHALL BE BUSHED WITH APPROVED THREADED BUSHINGS BEFORE INSTALLATION OF THE WIRE OR CABLE.

ALL METALLIC CONDUITS IN WHICH WIRE OR CABLE IS NOT TO BE INSTALLED SHALL BE CAPPED WITH THREADED PROTECTIVE CAPS, AS APPROVED BY THE ENGINEER.

ALL NONMETALLIC CONDUIT SHALL BE CAPPED OR PLUGGED IMMEDIATELY AFTER INSTALLATION AND SHALL REMAIN CAPPED OR PLUGGED UNTIL WIRE/CABLES ARE INSTALLED.

NONMETALLIC CONDUITS IN WHICH WIRE OR CABLE IS NOT BEING INSTALLED SHALL REMAIN CAPPED OR PLUGGED.

BENDING OF PVC ELECTRICAL CONDUIT SHALL BE ACCOMPLISHED BY USING A BLANKET OR EMERSON TYPE TANK DESIGNED FOR THE PURPOSE OF BENDING PVC ELECTRICAL CONDUIT.

ALL CUT ENDS SHALL BE TRIMMED INSIDE AND OUTSIDE TO REMOVE ALL ROUGH EDGES ON NONMETALLIC CONDUIT. (SEE NEC 347.5)

WHEN REQUIRED TO CONNECT NONMETALLIC CONDUIT TO METALLIC CONDUIT, ONLY U.L. LISTED ADAPTER FITTINGS SHALL BE USED.

PRIOR TO CONDUIT ACCEPTANCE, CONDUIT CAPS OR PLUGS SHALL BE REMOVED, AND THE CAPS, PLUGS AND CONDUIT ENDS SHALL BE THOROUGHLY CLEANED AND THEN THE CAPS OR PLUGS REINSTALLED TO ENSURE THAT THE CAPS OR PLUGS CAN BE EASILY REMOVED IN THE FUTURE.

ALL CONDUIT BEING FURNISHED AND INSTALLED SHALL HAVE THE U.L. LABEL FIRMLY ATTACHED.

CONDUIT RUNS SHALL BE THE SAME SIZE OF CONDUIT FROM ONE END TO THE OTHER (FROM PULL BOX TO PULL BOX-OR-JUNCTION BOX TO JUNCTION BOX-OR-BASE TO BASE, ETC.).

TRACER WIRE SHALL BE INSTALLED AS STATED IN THE STANDARD SPECIFICATION, ITEM 652.3.1.1.

ALL CONDUIT RUNS SHALL BE STRAIGHT (WITHOUT BENDS) FROM PULL BOX TO PULL BOX, PULL BOX TO BASE AND BASE TO BASE AS SHOWN ON THE PLANS.

6

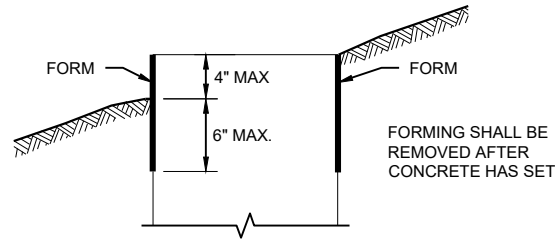
6

S.D.D. 9 B 2-10

S.D.D. 9 B 2-10

<b>CONDUIT</b>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED March, 2017 DATE	/S/ Ahmet Demirbilek STATE ELECTRICAL ENGINEER
FHWA	

FORM DEPTH SHALL BE NO MORE THAN 6" BELOW GRADE ON THE LOWER SIDE OF BASE



**FORMING DETAIL**

QUANTITY REQUIREMENTS	CONCRETE BASE TYPE		
	1	2	5 & 6
APPROX. CUBIC YARDS OF CONCRETE	0.40	0.57	0.40
LBS. OF HOOP BAR STEEL	NONE	23	16
LBS. OF VERTICAL BAR STEEL	NONE	60	18

**GENERAL NOTES**

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

BASES SHALL BE EXCAVATED BY USE OF A CIRCULAR AUGER.

TOP SURFACES OF CONCRETE BASES SHALL BE TROWEL FINISHED SMOOTH AND LEVEL.

CONDUIT SIZES AND LOCATIONS SHALL BE SHOWN ON THE PLANS

THE FINAL OR TERMINATING CONCRETE BASE IN A CONDUIT RUN SHALL HAVE A 6" EXIT STUB INSTALLED FOR FUTURE CABLING USE. THE EXIT STUB SHALL BE SIZED AS USED THROUGHOUT THE CONDUIT RUN AS SHOWN AT THE ENTRANCE OF THE BASE.

ENDS OF CONDUIT INSTALLED BELOW GRADE FOR FUTURE USE SHALL BE CAPPED IF METALLIC OR PLUGGED IF NON-METALLIC.

MINIMUM BENDING RADIUS OF CONDUIT IS EQUAL TO 6X THE DIAMETER.

CONDUIT HEIGHT ABOVE CONCRETE BASES SHALL BE 1 INCH. ALL METALLIC CONDUIT ENDS SHALL BE REAMED AND THREADED.

ALL CONDUIT ENDS AT THE TOP OF CONCRETE BASES SHALL BE CAPPED IF METALLIC OR PLUGGED IF NON-METALLIC IMMEDIATELY AFTER PLACEMENT AND BEFORE CONCRETE IS POURED. CONDUITS IN WHICH WIRE OR CABLE IS NOT INSTALLED SHALL REMAIN CAPPED OR PLUGGED.

BELL ENDS SHALL BE INSTALLED ON ALL PVC CONDUIT EXPOSED AT THE TOP OF CONCRETE BASES BEFORE INSTALLATION.

WHEN REQUIRED TO CONNECT NON-METALLIC CONDUIT TO METALLIC CONDUIT, ONLY ADAPTER FITTINGS, U.L. LISTED FOR ELECTRICAL USE, SHALL BE USED.

IF A BASE REQUIRES A DEEP FORM BECAUSE OF LOOSE DIRT OR FILL, THE FORM SHALL BE REMOVED BEFORE BACKFILLING AROUND THE BASE. BACKFILL SHALL BE TAMPED TIGHT AGAINST THE BARE CONCRETE BASE IN LAYERS OF 1 FOOT OR LESS.

A NO. 4 AWG STRANDED COPPER EQUIPMENT GROUNDING CONDUCTOR SHALL BE EXOTHERMICALLY WELDED TO THE EQUIPMENT GROUNDING ELECTRODE (GROUND ROD) FOR TYPE 2, TYPE 5 AND TYPE 6 BASES.

THE EQUIPMENT GROUNDING CONDUCTOR SHALL BE FURNISHED AND INSTALLED TO ENTER ALL BASE TYPES THROUGH A 1 INCH CONDUIT INSTALLED FOR GROUNDING PURPOSES, LEAVING A 4 FOOT COIL OF WIRE ABOVE THE CONCRETE BASE. THE EQUIPMENT GROUNDING CONDUCTOR SHALL BE NEATLY COILED AND THE COILS TIED TOGETHER.

ANCHOR RODS SHALL BE THREADED 12" IN LENGTH ON EACH END OF THE ROD. ANCHOR RODS SHALL BE MANUFACTURED IN ACCORDANCE WITH SECTION 654.2.1 OF THE STANDARD SPECIFICATIONS.

WASHERS AND LOCK WASHERS ARE REQUIRED ON ALL ANCHOR RODS.

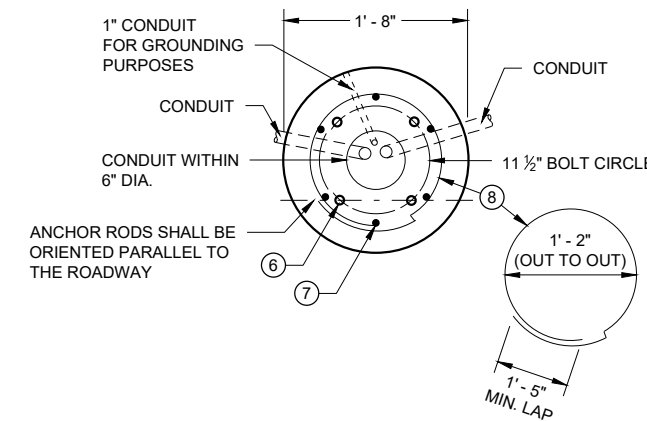
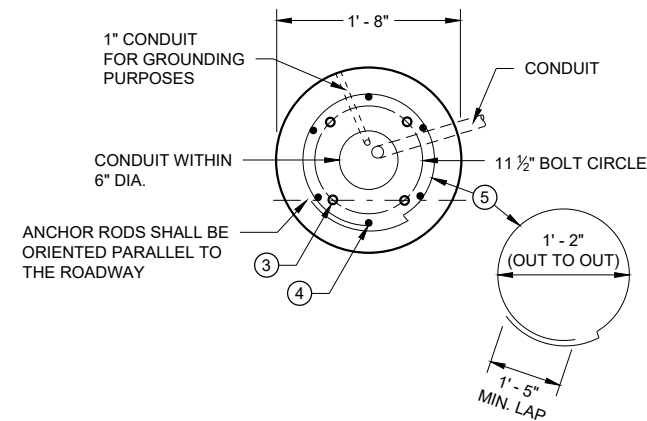
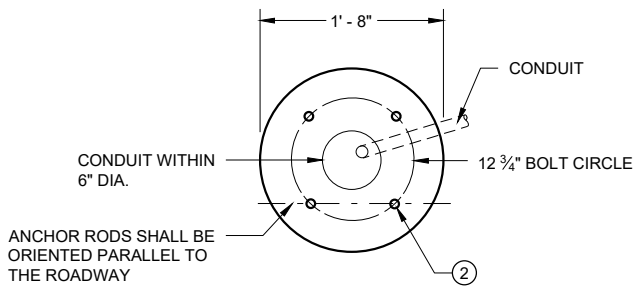
WHEN ANCHOR RODS USING THE ALTERNATE "L" BEND ARE FURNISHED, THE 4 INCH "L" BEND SHALL BE IN ADDITION TO THE SPECIFIED ANCHOR ROD BAR LENGTH. THE "L" BEND SHALL NOT BE THREADED.

ANCHOR RODS SHALL BE INSTALLED WITH MISALIGNMENTS OF LESS THAN 1:40 FROM VERTICAL.

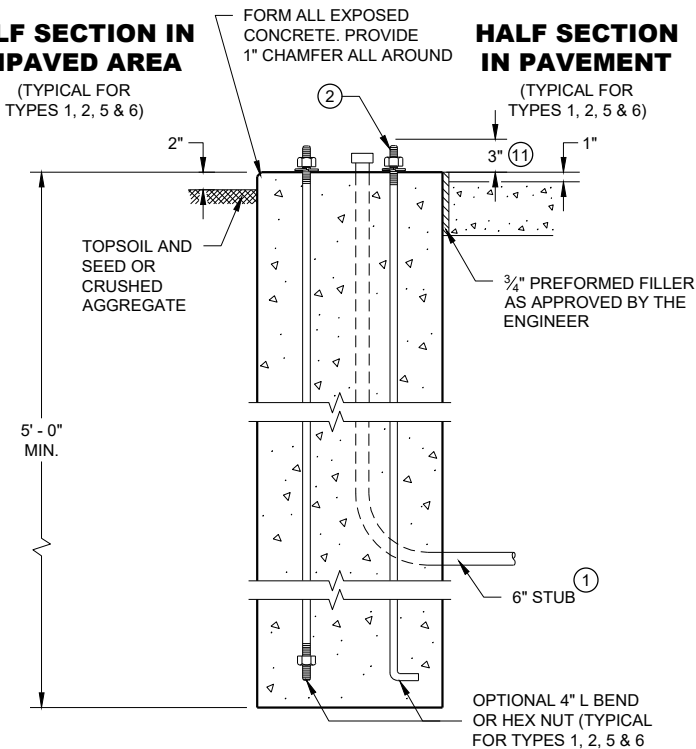
WELDING OF THE ANCHOR RODS TO THE CAGE IS UNACCEPTABLE. TIE WIRES SHALL BE USED.

BAR STEEL REINFORCEMENT SHALL BE COATED WITH POWDERED EPOXY RESIN IN ACCORDANCE WITH SECTION 505 OF THE STANDARD SPECIFICATIONS (LATEST EDITION).

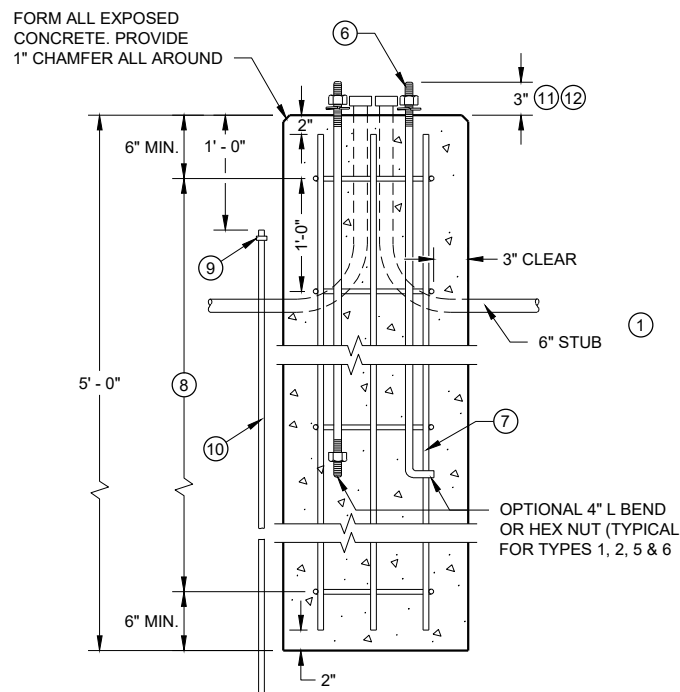
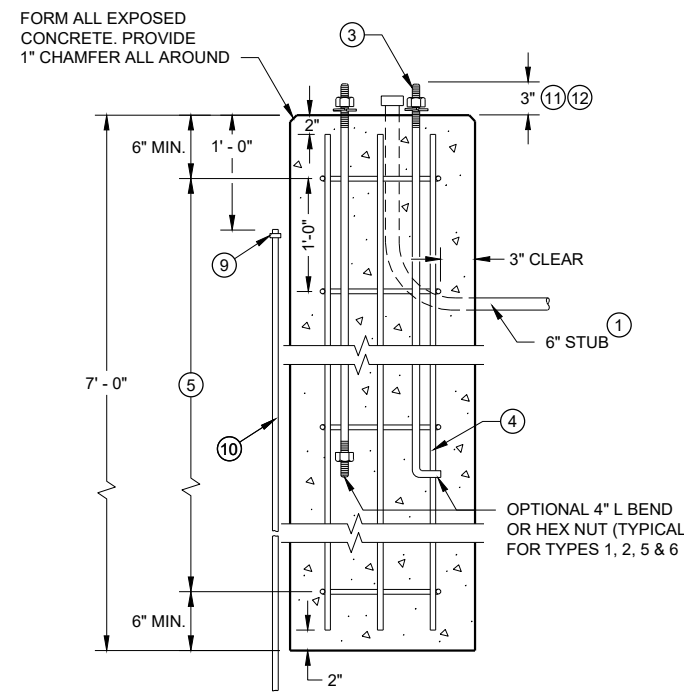
- ① THE MINIMUM DEPTH OF CONDUIT EXITING THE CONCRETE BASE AND INSTALLED BELOW THE TRAVELED WAY SHALL BE 24 INCHES. THE MINIMUM DEPTH OF CONDUIT EXITING THE CONCRETE BASE THAT IS NOT INSTALLED BELOW THE TRAVELED WAY SHALL BE 18 INCHES. THE MAXIMUM DEPTH OF ALL CONDUIT SHALL BE 36 INCHES EXCEPT WITH WRITTEN APPROVAL OF THE ENGINEER.
- ② (4) 1" DIA. X 3' - 6" ANCHOR RODS.
- ③ (4) 1" DIA. X 5' - 0" ANCHOR RODS.
- ④ (6) NO. 6 X 6' - 8" BAR STEEL REINFORCEMENT.
- ⑤ (7) NO. 4 X 5' - 1" BAR STEEL REINFORCEMENT @ 1' - 0" C - C.
- ⑥ (4) 1" DIA. X 3' - 6" ANCHOR RODS.
- ⑦ (6) NO. 4 X 4' - 8" BAR STEEL REINFORCEMENT.
- ⑧ (5) NO. 4 X 5' - 1" BAR STEEL REINFORCEMENT @ 1' - 0" C - C.
- ⑨ EXOTHERMIC CONNECTION TO EQUIPMENT GROUNDING CONDUCTOR
- ⑩ 5/8" DIA. X 8' - 0" COPPERCLAD EQUIPMENT GROUNDING ELECTRODE REQUIRED
- ⑪ ANY ANCHOR ROD PROJECTION SHORTER THAN 2 3/4" OR LONGER THAN 3 1/4" SHALL REQUIRE THE BASE TO BE REMOVED AND REPLACED AT THE CONTRACTORS EXPENSE.
- ⑫ FOR NON - BREAKAWAY INSTALLATIONS, 4 1/2" ± ANCHOR ROD PROJECTION WITH THE USE OF LEVELING NUTS. RODENT SCREEN REQUIRED.



**HALF SECTION IN UNPAVED AREA**



**HALF SECTION IN PAVEMENT**



**CONCRETE BASES**

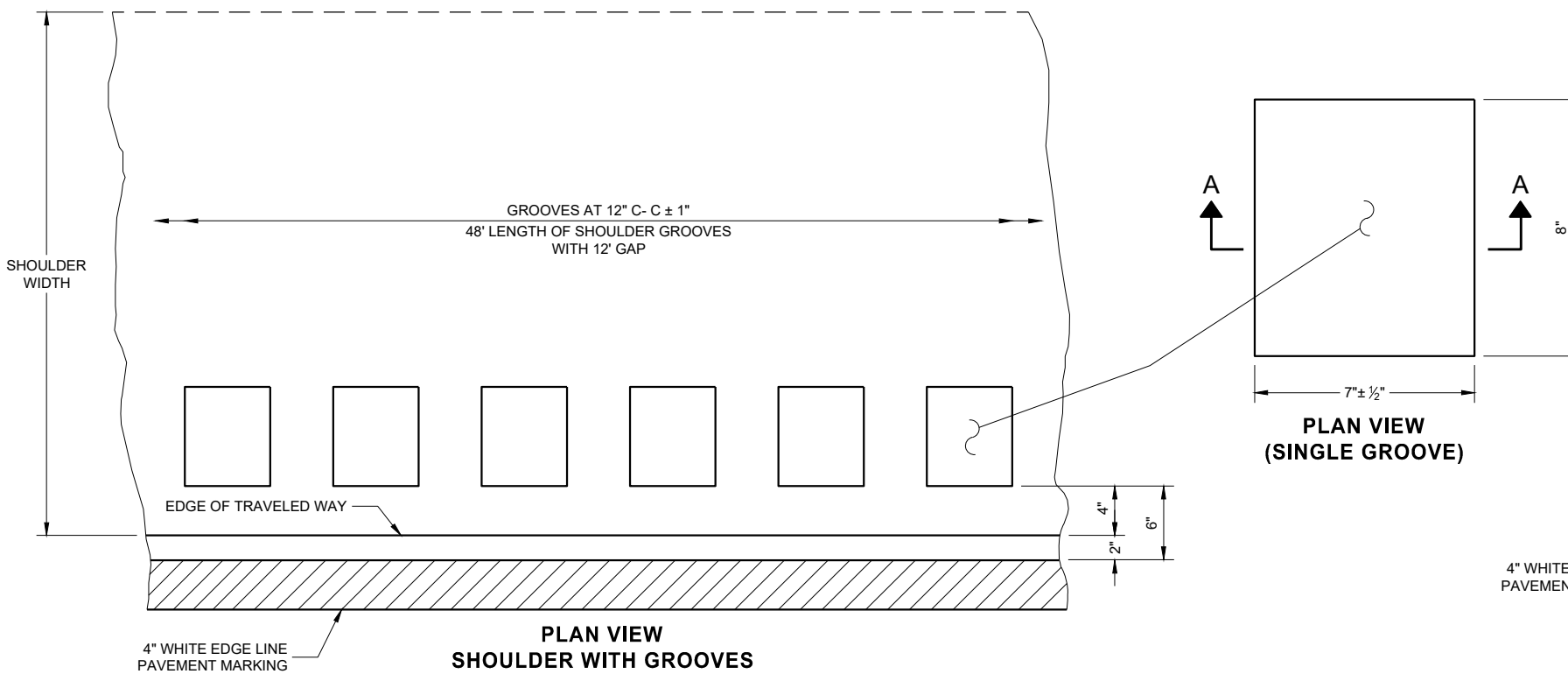
**CONCRETE BASES  
TYPES 1, 2, 5, & 6**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
May 2019 /S/ Ahmet Demirelek  
DATE STATE ELECTRICAL ENGINEER

FHWA





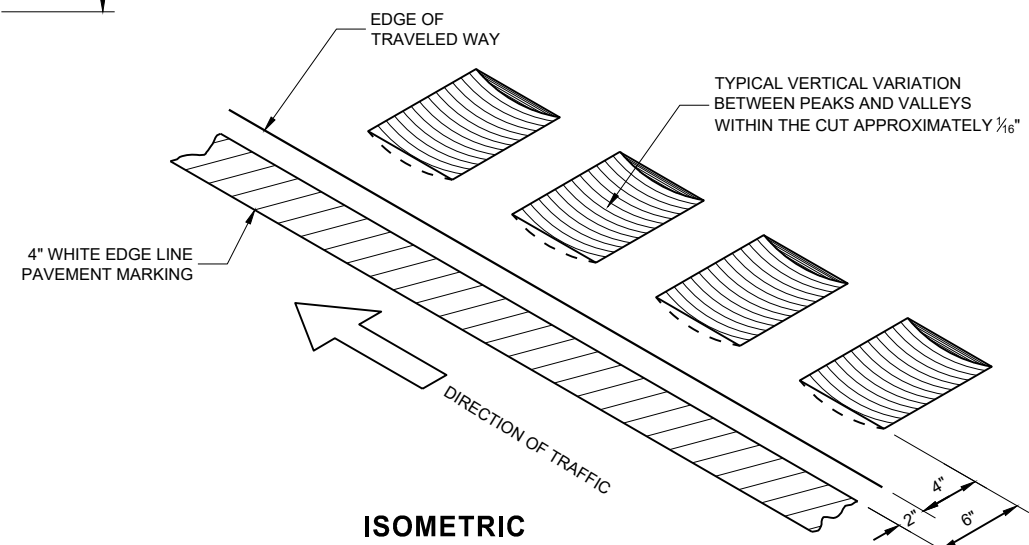
PLACEMENT DETAIL FOR TYPE 1 MILLED RUMBLE STRIP

**GENERAL NOTES**

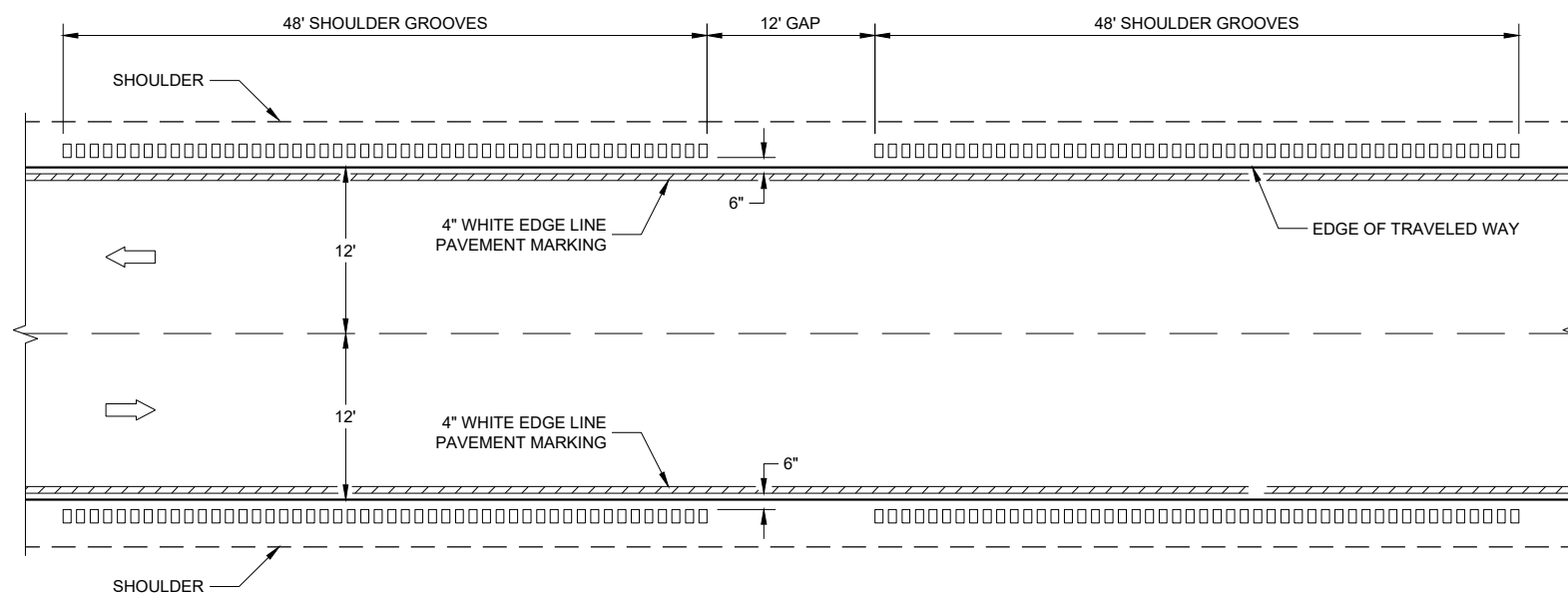
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

DO NOT MILL SHOULDER GROOVES THROUGH ANY INTERSECTION, MARKED CROSSWALK, NON-MOTORIZED PATH CROSSING, OR SNOWMOBILE CROSSING.

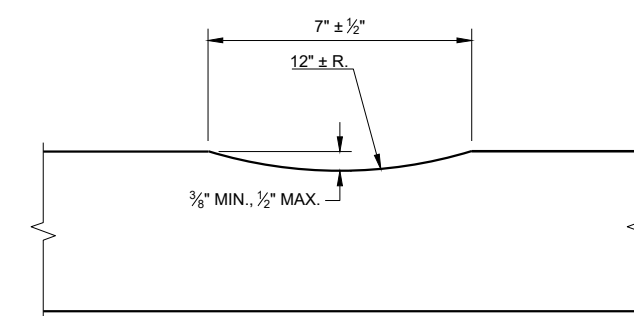
- ① SHOULDER GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS. WHEN DIRECTED BY THE ENGINEER.



ISOMETRIC



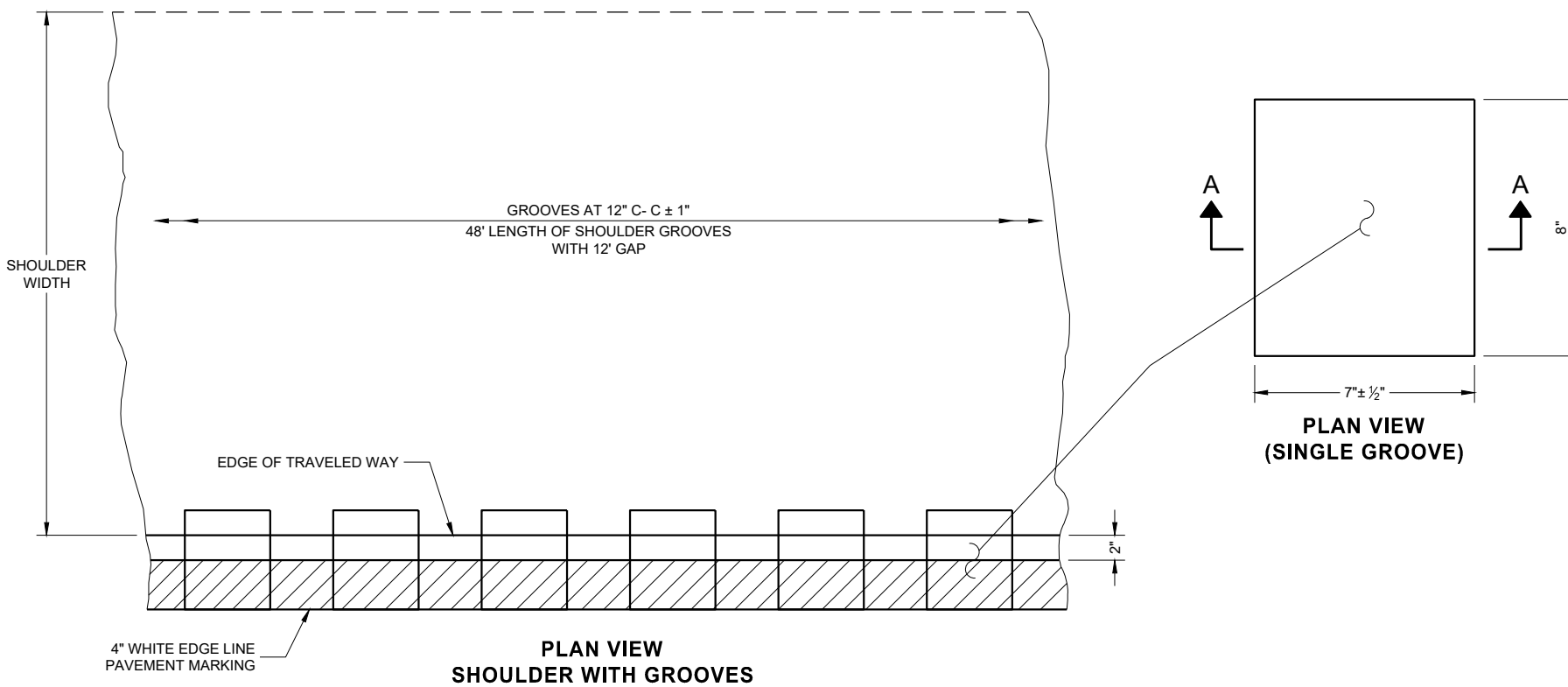
TYPE 1  
2 - LANE SHOULDER RUMBLE STRIP



SECTION A - A

**2-LANE RURAL SHOULDER  
RUMBLE STRIP, MILLING**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION



6

6

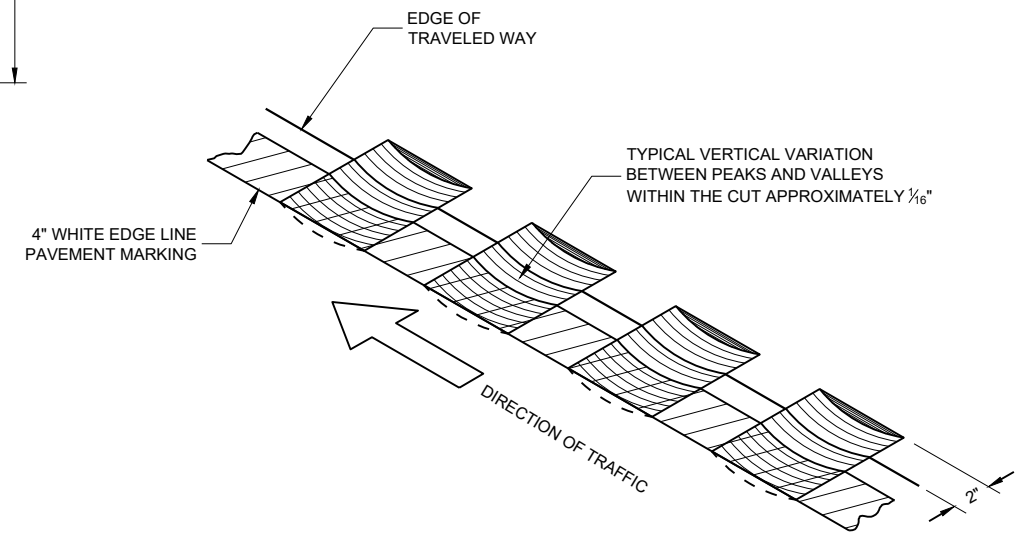
PLACEMENT DETAIL FOR TYPE 2 MILLED RUMBLE STRIP

**GENERAL NOTES**

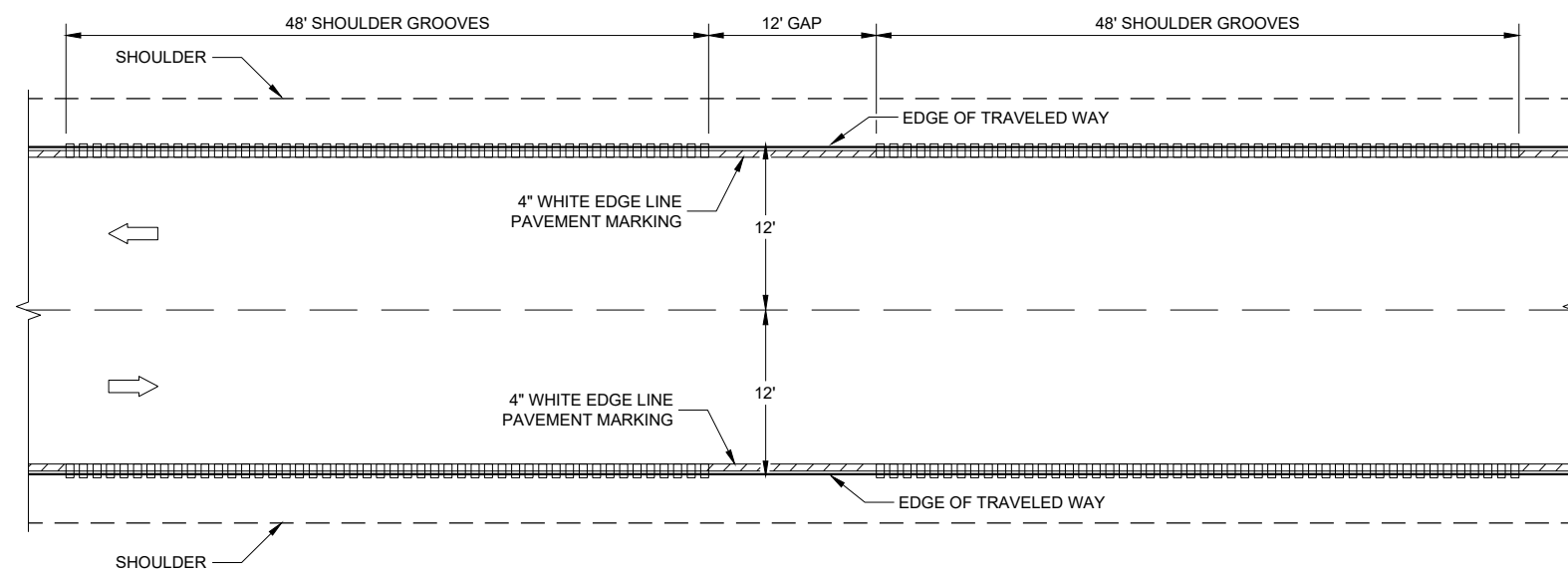
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

DO NOT MILL SHOULDER GROOVES THROUGH ANY INTERSECTION, MARKED CROSSWALK, NON-MOTORIZED PATH CROSSING, OR SNOWMOBILE CROSSING.

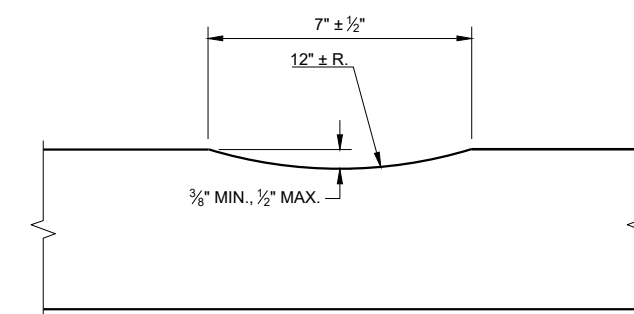
- ① SHOULDER GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS. WHEN DIRECTED BY THE ENGINEER.



ISOMETRIC



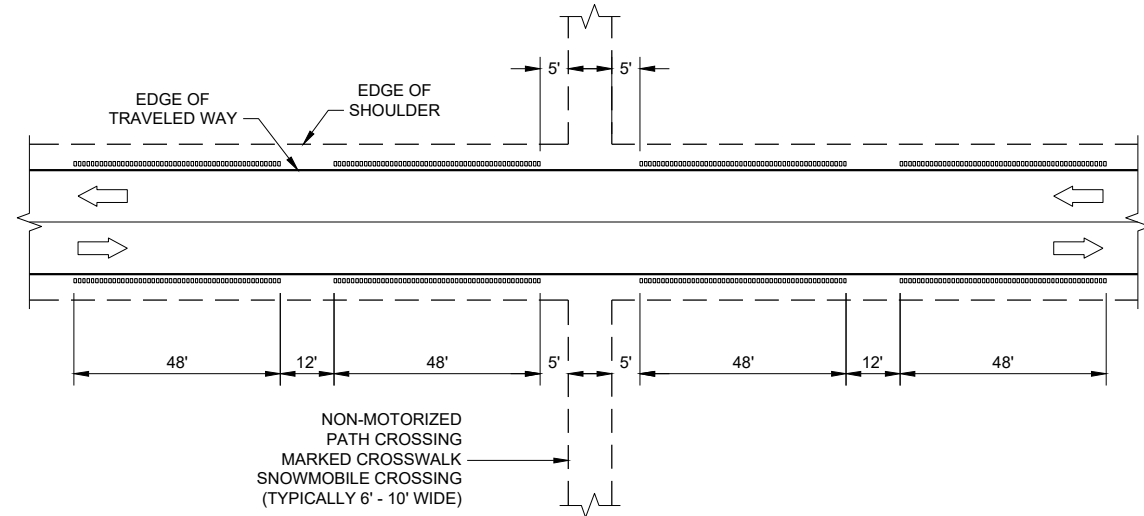
TYPE 2  
2 - LANE SHOULDER RUMBLE STRIP



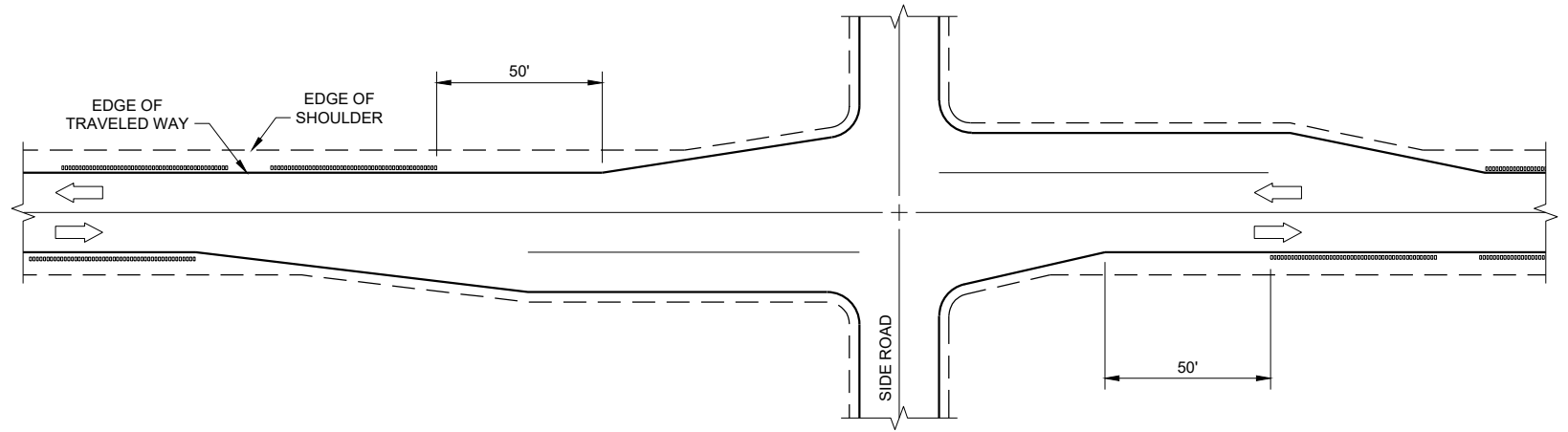
SECTION A - A

**2-LANE RURAL SHOULDER  
RUMBLE STRIP, MILLING**

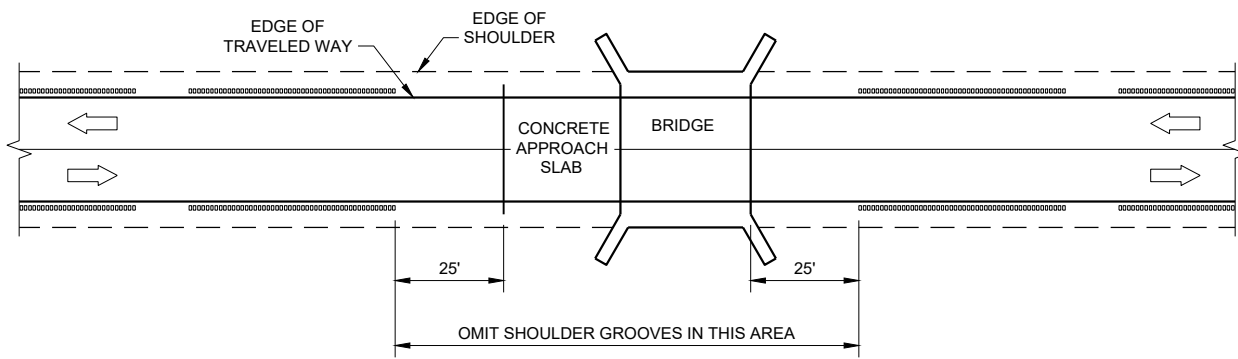
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION



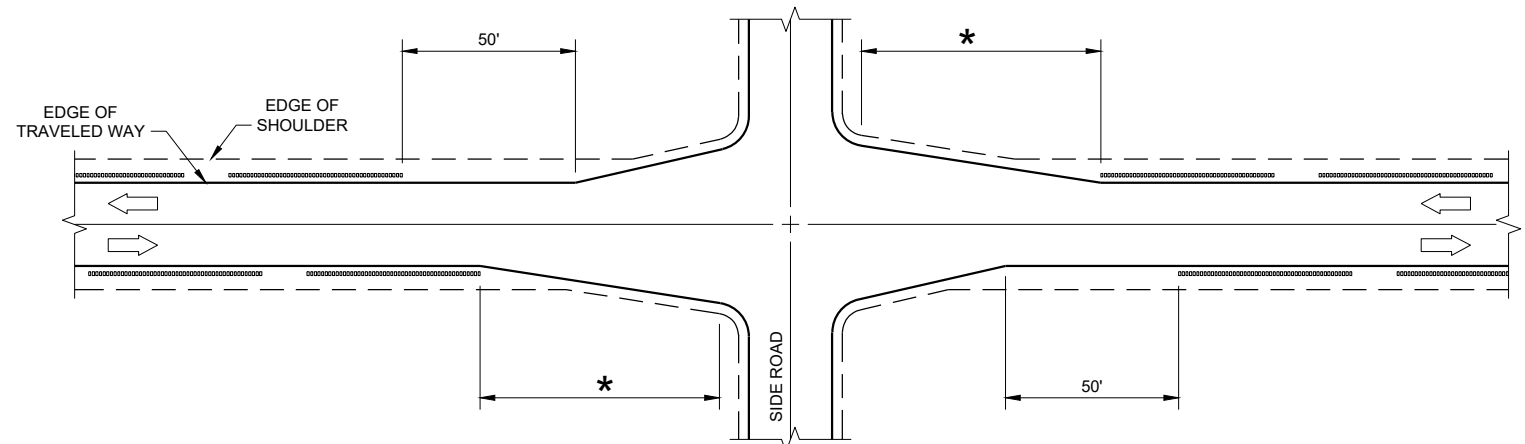
**SHOULDER GROOVES AT MISCELLANEOUS CROSSINGS**



**SHOULDER GROOVES AT RIGHT TURN LANE**

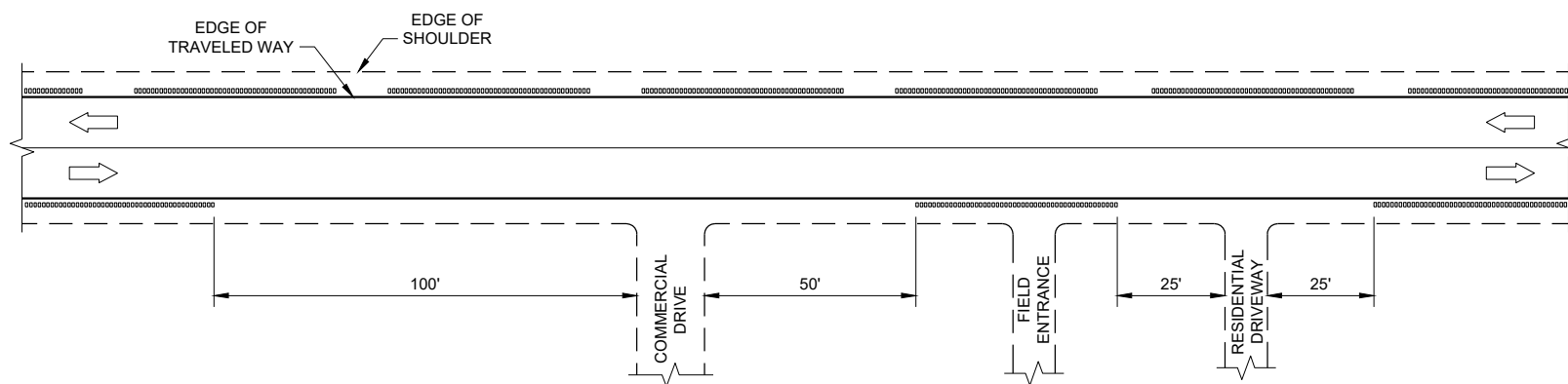


**SHOULDER GROOVES AT BRIDGES**



**SHOULDER GROOVES AT INTERSECTIONS WITH APPROACH TAPER**

\* GREATER OF 100' OR APPROACH TAPER LENGTH



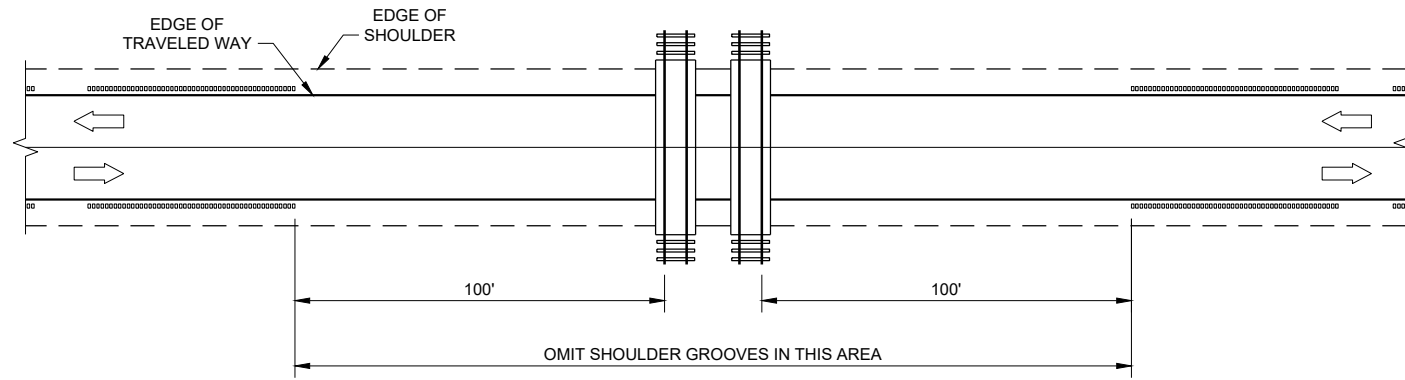
**SHOULDER GROOVES AT DRIVEWAYS<sup>①</sup>**

**GENERAL NOTES**

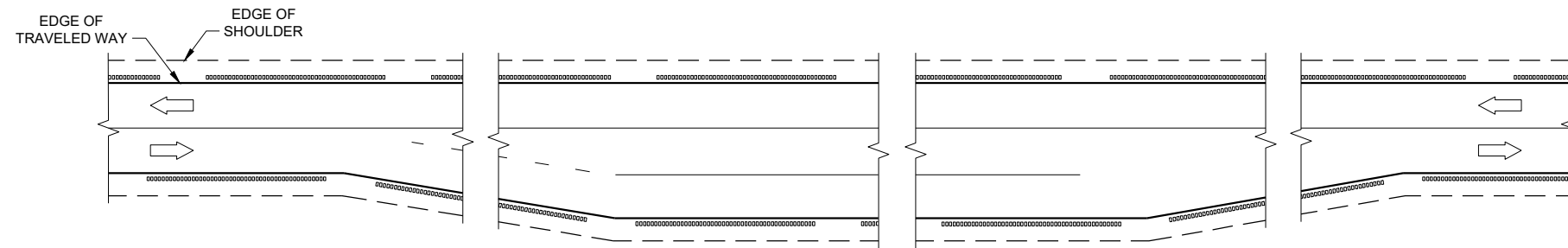
- ① SHOULDER GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS. WHEN DIRECTED BY THE ENGINEER.

**2-LANE RURAL SHOULDER  
RUMBLE STRIP, MILLING**

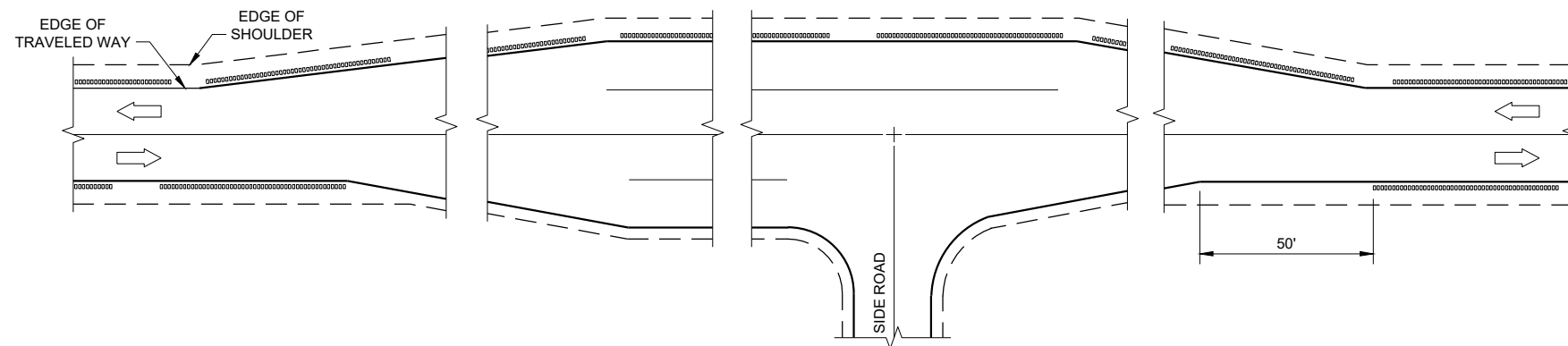
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION



**SHOULDER GROOVES AT RAILROADS**



**SHOULDER GROOVES AT PASSING AND CLIMBING LANES**



**SHOULDER GROOVES AT BYPASS LANES**

**2-LANE RURAL SHOULDER  
RUMBLE STRIP, MILLING**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
7/2018 DATE /S/ Rodney Taylor  
ROADWAY STANDARDS DEVELOPMENT  
ENGINEER

**GENERAL NOTES**

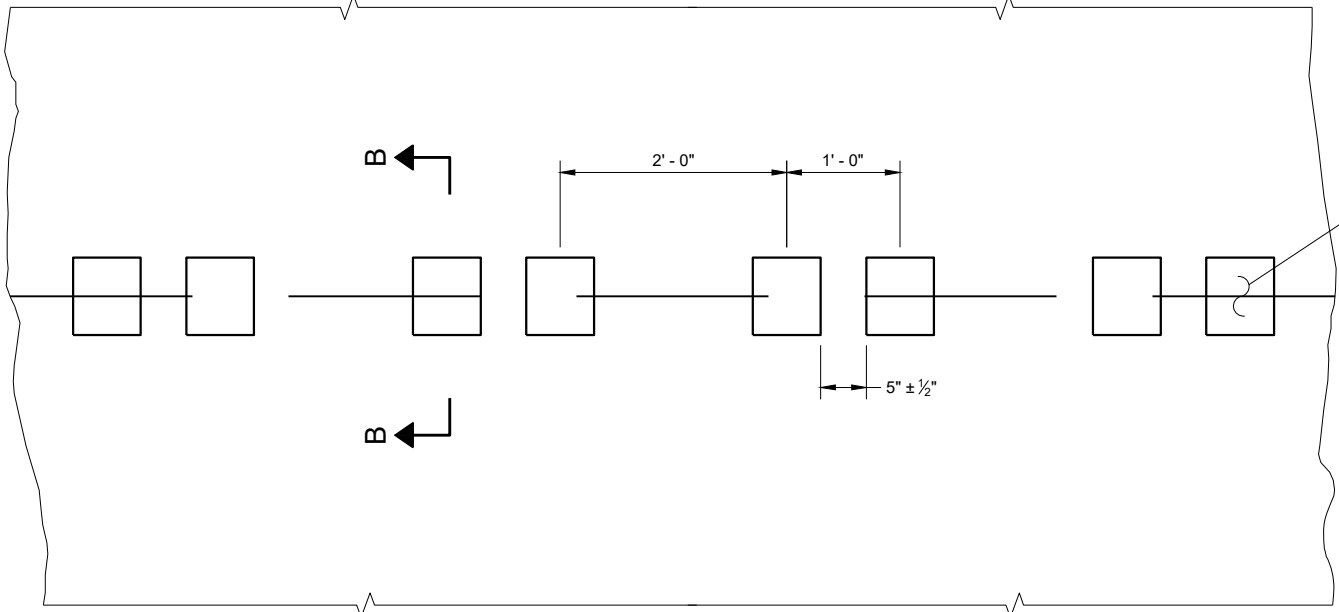
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

DO NOT MILL CENTERLINE GROOVES THROUGH ANY INTERSECTION, MARKED CROSSWALK, NON-MOTORIZED PATH CROSSING, OR SNOWMOBILE CROSSING.

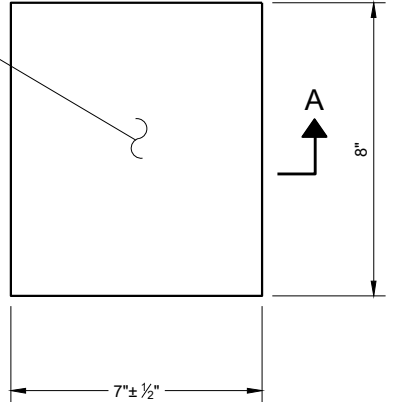
INSTALL PAVEMENT MARKING AFTER THE GROOVES ARE INSTALLED.

SEE SIGNING PLAN FOR SIGN REQUIREMENTS THAT MAY BE NEEDED.

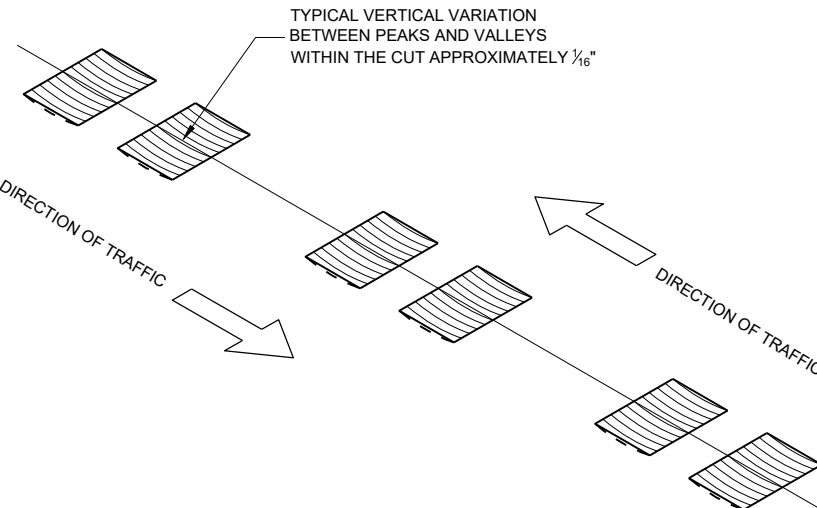
- ① CENTERLINE GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS, WHEN DIRECTED BY THE ENGINEER.



**PLAN VIEW  
SHOULDER WITH GROOVES**

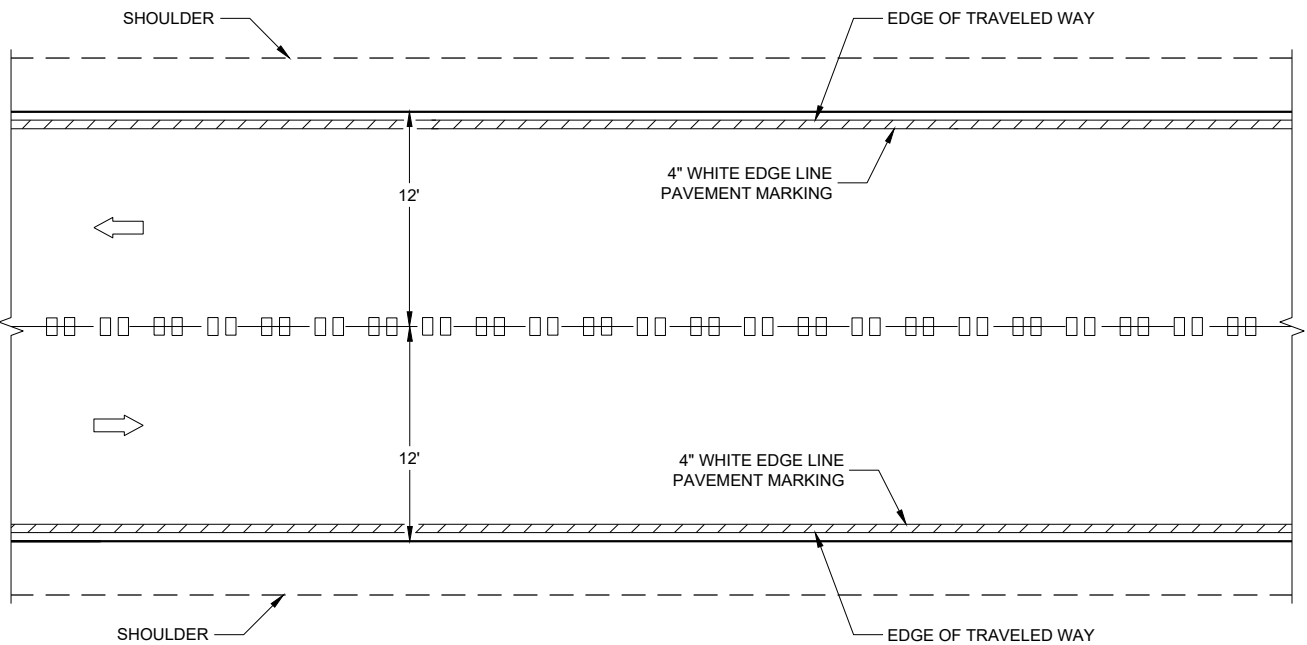


**PLAN VIEW  
(SINGLE GROOVE)**

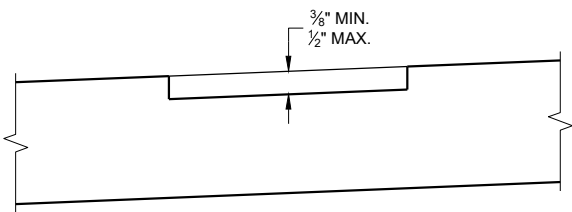


**ISOMETRIC**

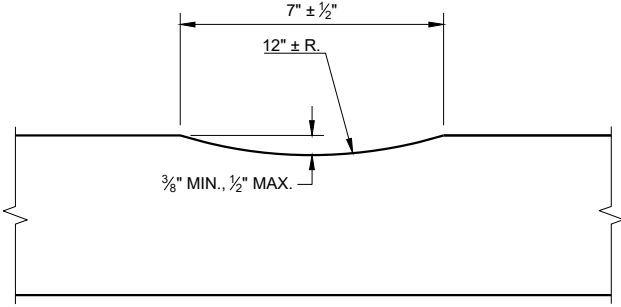
**PLACEMENT DETAIL FOR TYPE 1 MILLED RUMBLE STRIP**



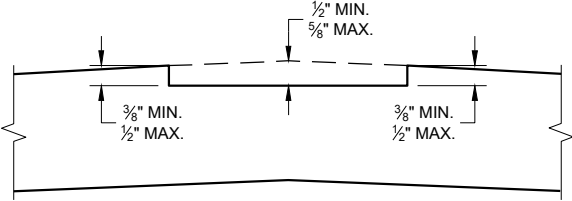
**CENTERLINE GROOVES ON TWO-WAY ROADWAYS**



**SECTION B - B  
SUPERELEVATED ROADWAY**



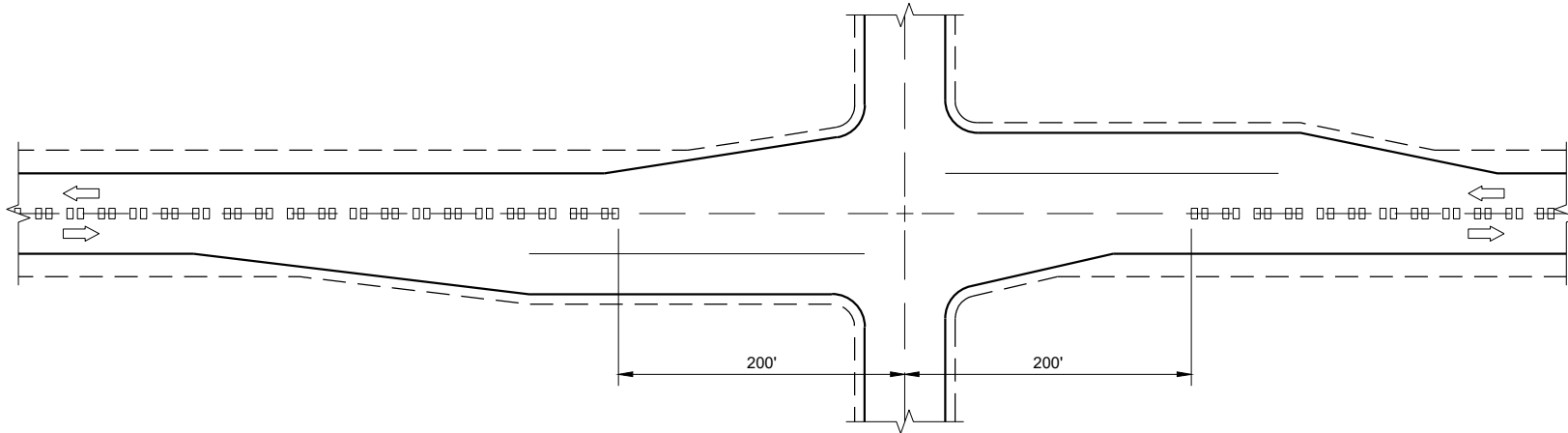
**SECTION A - A**



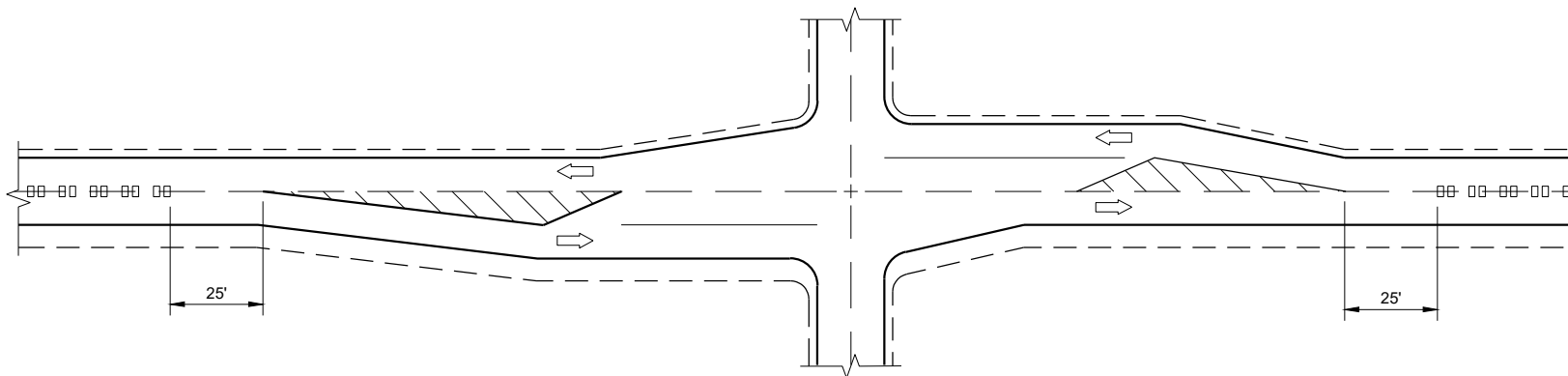
**SECTION B - B  
CROWNED ROADWAY**

**2-LANE RURAL  
CENTER LINE RUMBLE STRIP,  
MILLING**

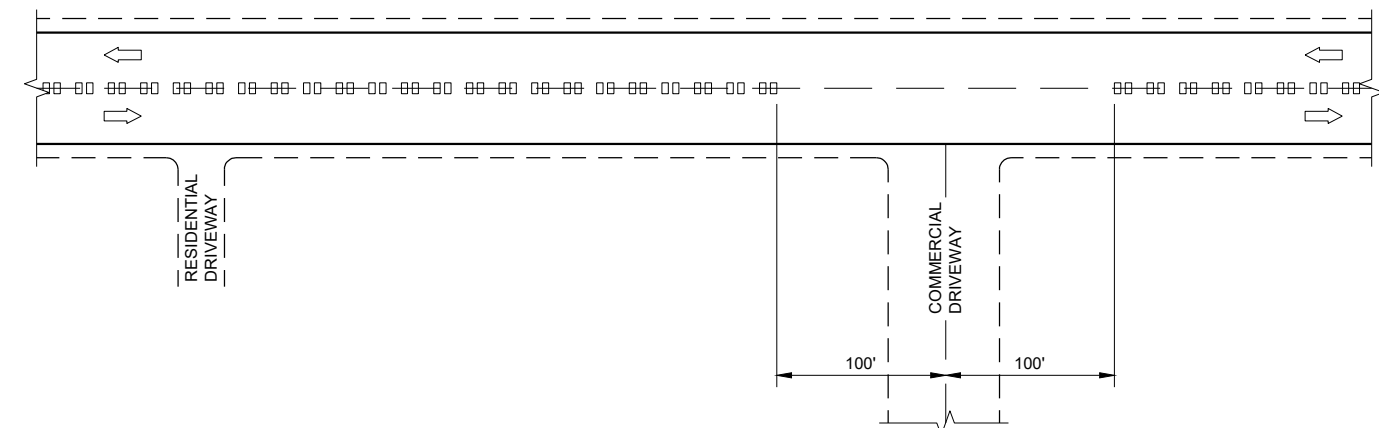
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION



**CENTERLINE GROOVES AT INTERSECTIONS**



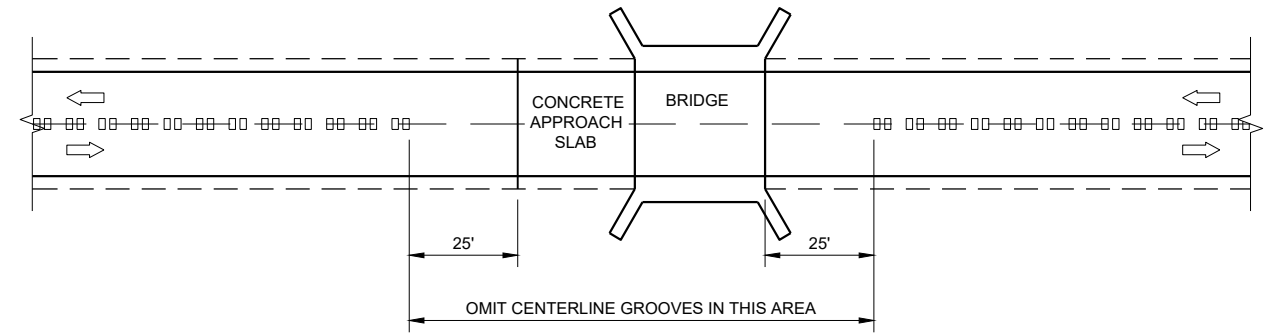
**CENTERLINE GROOVES AT INTERSECTIONS  
(WITH LEFT TURN LANES)**



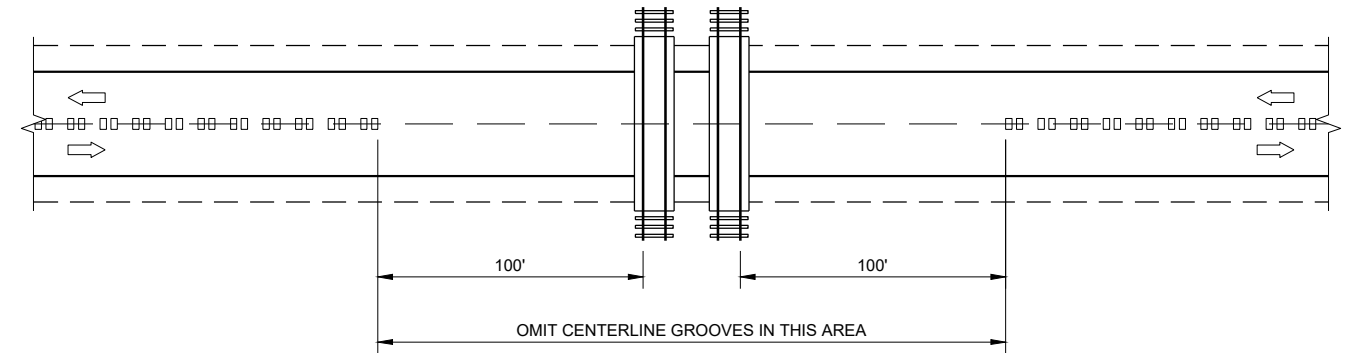
**CENTERLINE GROOVES AT DRIVEWAYS<sup>①</sup>**

**GENERAL NOTES**

- ① CENTERLINE GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS. WHEN DIRECTED BY THE ENGINEER.



**CENTERLINE GROOVES AT BRIDGES**



**CENTERLINE GROOVES AT RAILROADS**

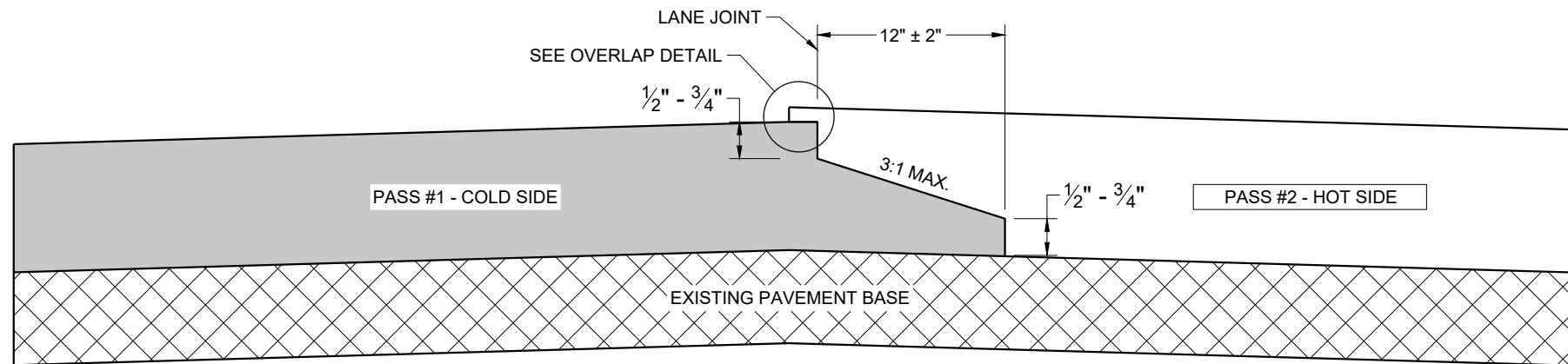
6

6

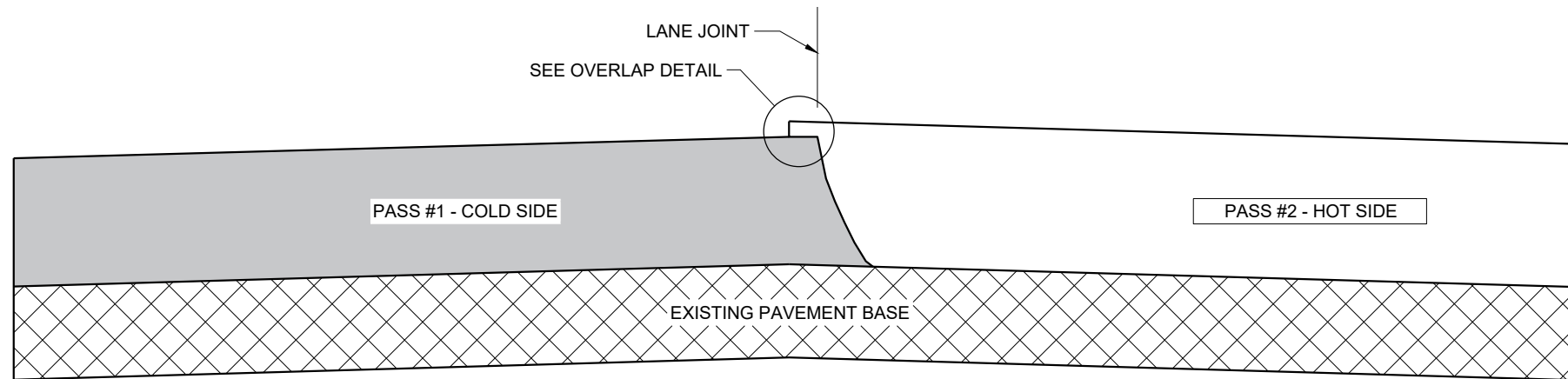
SDD 13A11 - 03b

SDD 13A11 - 03b

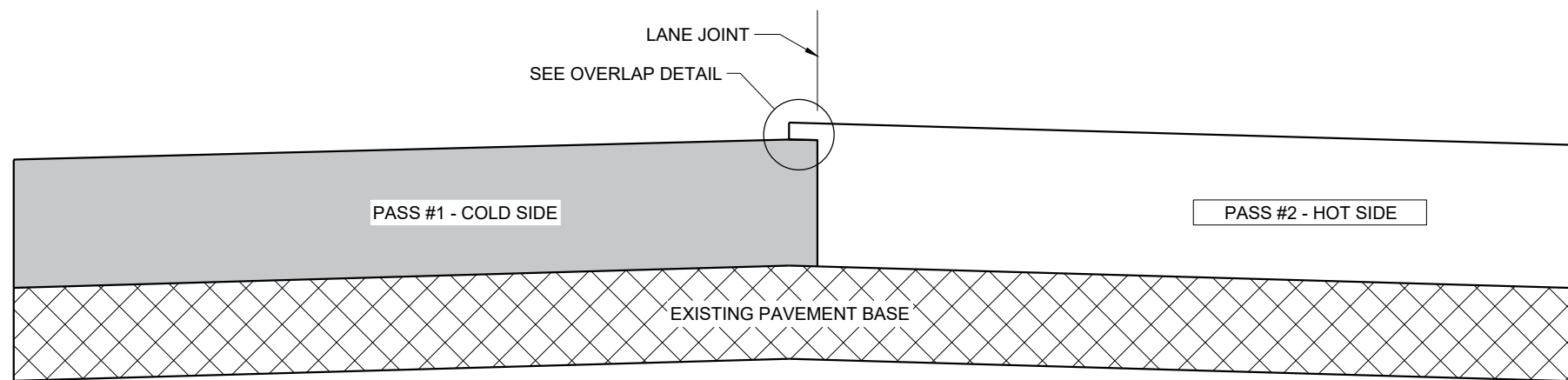
<b>2-LANE RURAL CENTERLINE RUMBLE STRIP, MILLING</b>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 7/2018 DATE	/S/ Rodney Taylor ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA	



**TYPICAL PAVEMENT CROSS SECTION  
NOTCHED WEDGE JOINT**



**TYPICAL PAVEMENT CROSS SECTION  
VERTICAL JOINT**



**TYPICAL PAVEMENT CROSS SECTION  
VERTICAL JOINT (MILLED)**

**GENERAL NOTES**

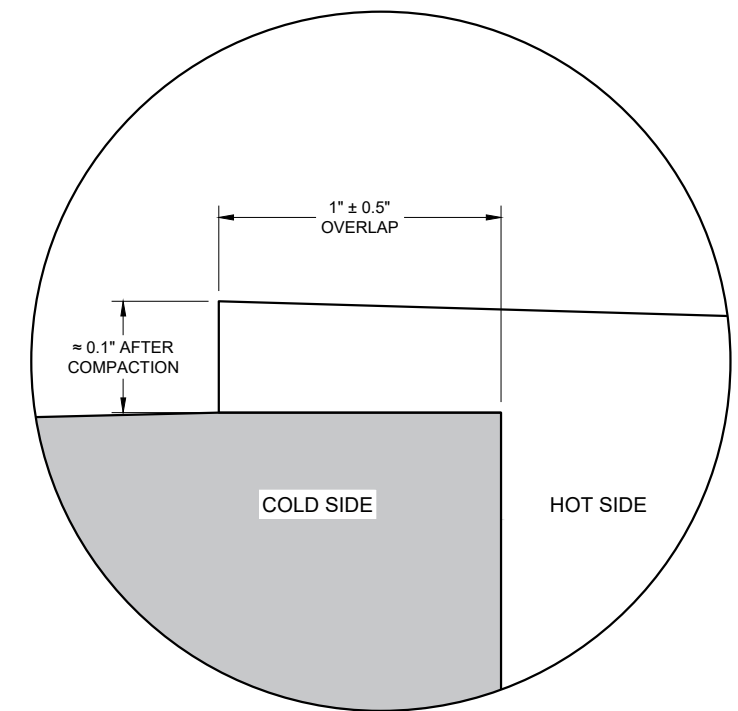
IN ADDITION TO THE DETAILS PROVIDED IN THIS DRAWING, CONFORM TO STANDARD SPECIFICATION 450.3.2.8 FOR WHEN A NOTCHED WEDGE JOINT IS REQUIRED AND FOR GENERAL JOINT CONSTRUCTION REQUIREMENTS.

FOR ALL LONGITUDINAL JOINTS, ENSURE THE PAVER SCREED OVERLAPS THE PREVIOUSLY PLACED PAVEMENT BY  $1" \pm 0.5"$  AND THE HOT SIDE OF THE JOINT REMAINS HIGHER THAN THE COLD SIDE BY APPROXIMATELY 0.1" AFTER FINAL COMPACTION. (IT WILL BE FLUSH WHEN PAVING IN ECHELON.)

ONLY REMOVE THE LONGITUDINAL NOTCHED WEDGE JOINT FOR SMA PAVEMENT OR AS DIRECTED BY THE ENGINEER TO ADDRESS SPECIFIC LENGTHS OF JOINT DAMAGED BY TRAFFIC.

WHEN MILLING BACK OR REMOVING ANY LONGITUDINAL JOINT, LIMIT THE MATERIAL REMOVED TO 2" FROM THE TOP NOTCH OR FROM THE VERTICAL JOINT EDGE ON THE COLD SIDE OF THE JOINT.

USE LONGITUDINAL MILLED JOINT AS PLANS SHOW OR THE AS THE ENGINEER DIRECTS.



**OVERLAP DETAIL (TYPICAL)**

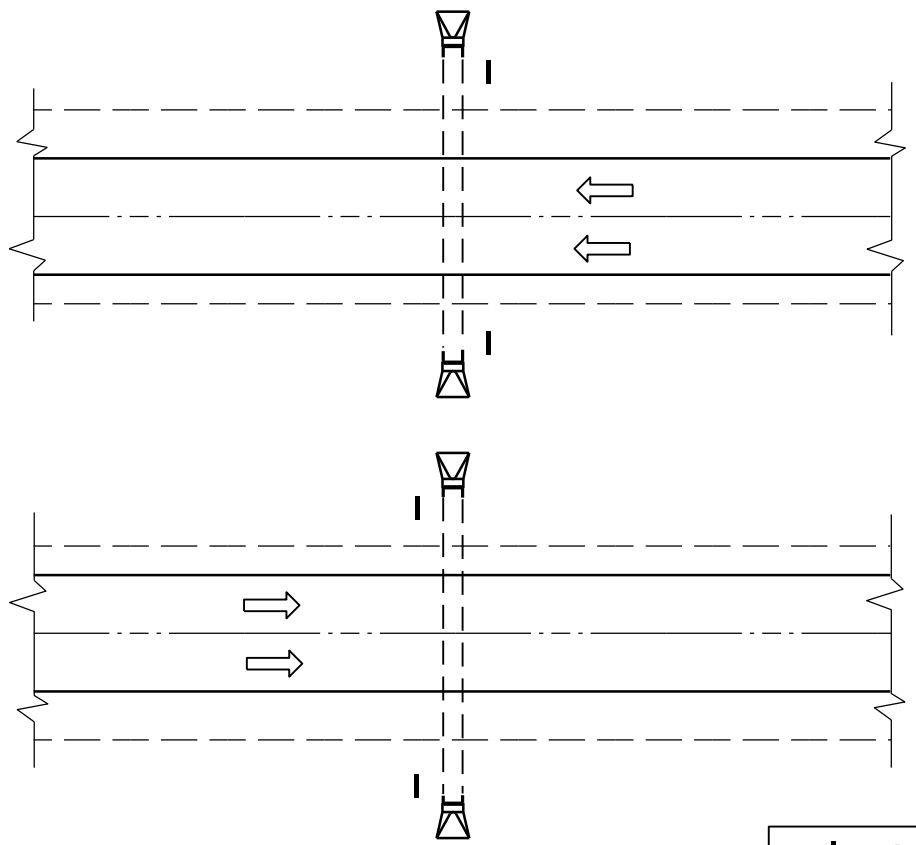
6

6

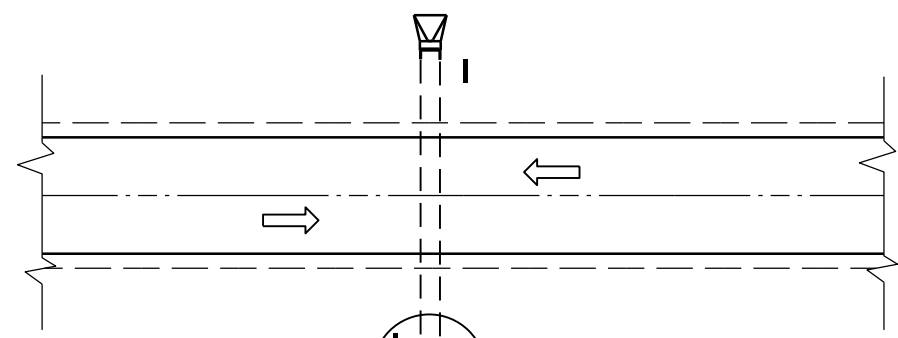
SDD 13C19 - 03

SDD 13C19 - 03

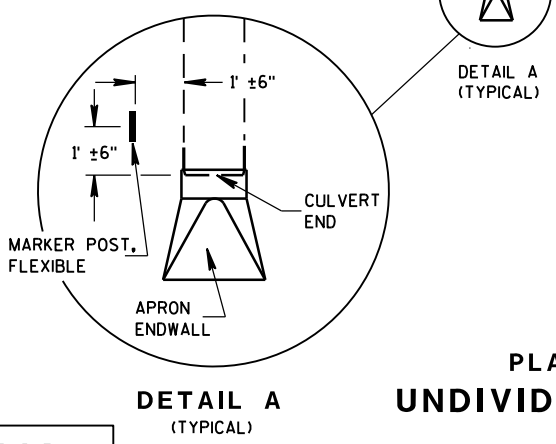
<b>HMA LONGITUDINAL JOINTS</b>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED November 2020 DATE	/S/ Steven Hefel HMA PAVEMENT ENGINEER
FHWA	



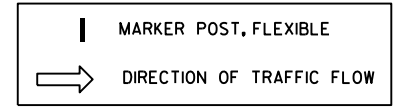
PLAN VIEW  
DIVIDED HIGHWAY



PLAN VIEW  
UNDIVIDED HIGHWAY



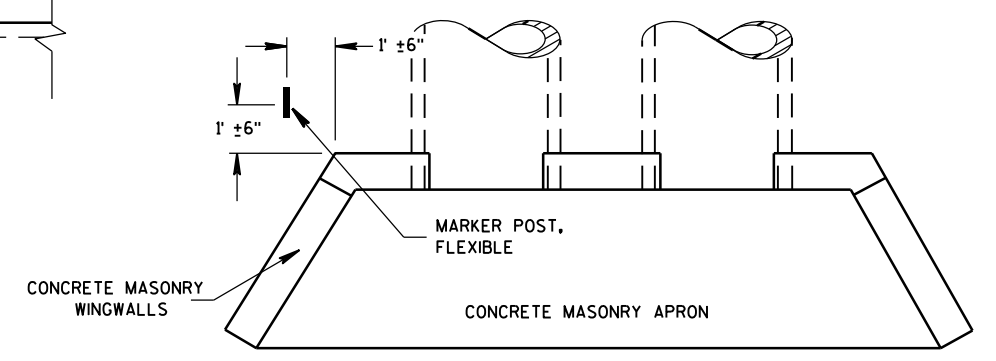
DETAIL A  
(TYPICAL)



FLEXIBLE MARKER POST LOCATION

GENERAL NOTES

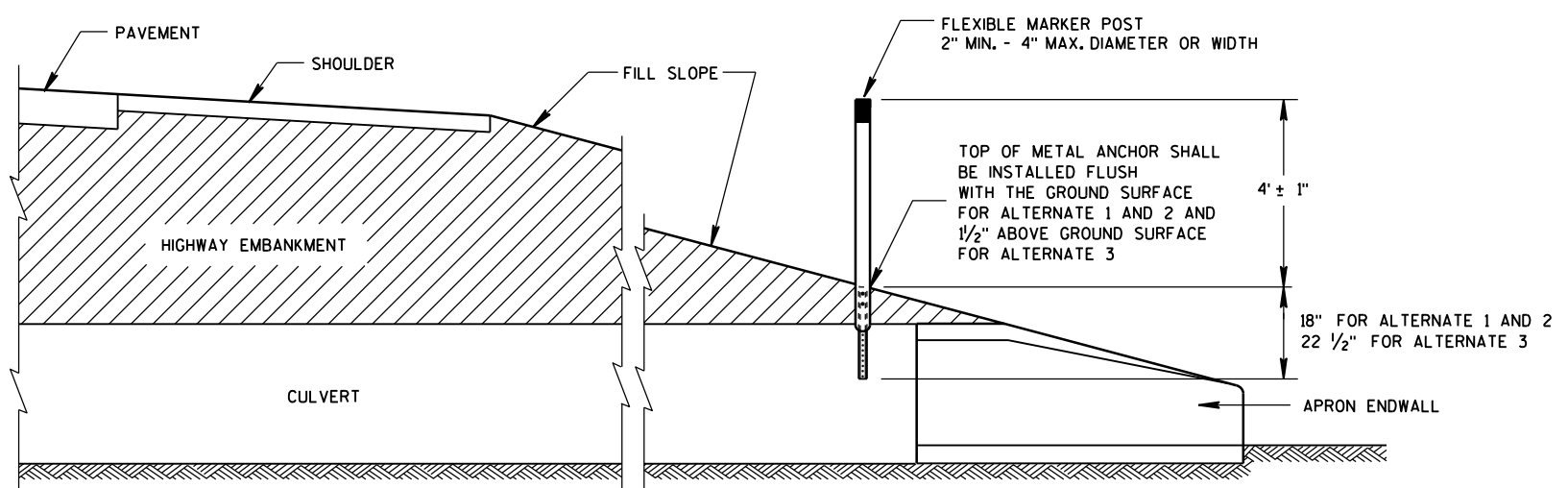
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.



PLAN VIEW  
CONCRETE MASONRY ENDWALLS FOR  
CULVERT PIPE AND PIPE ARCH

6

6



CROSS SECTION  
FLEXIBLE MARKER POST

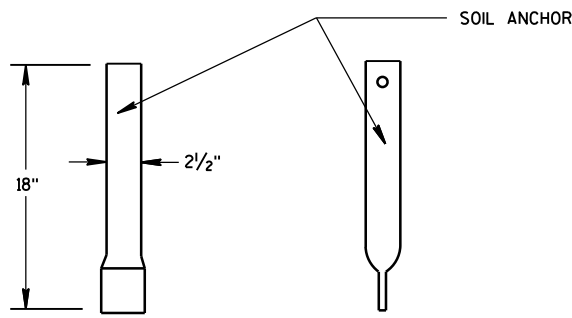
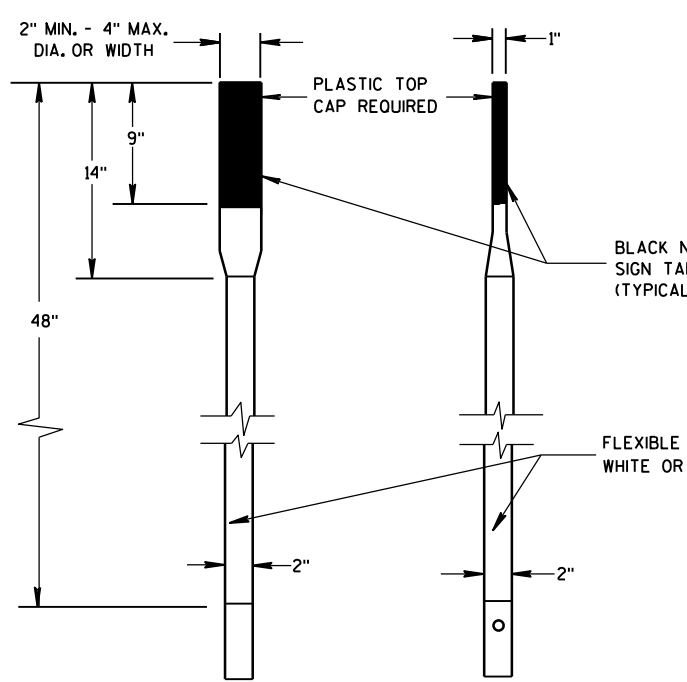
FLEXIBLE MARKER POST  
FOR CULVERT END

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

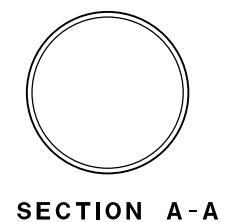
S.D.D. 15 A 3-2a

S.D.D. 15 A 3-2a

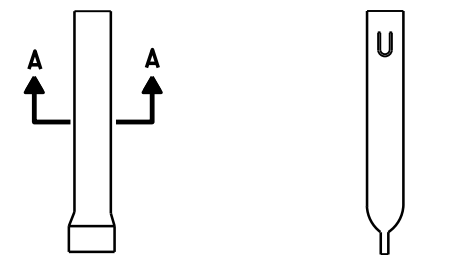




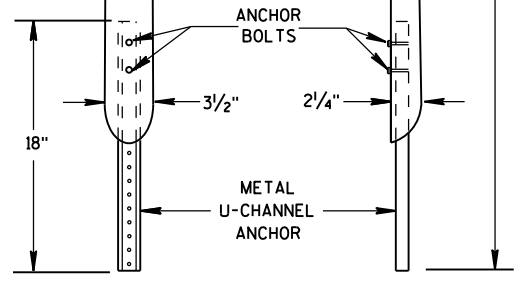
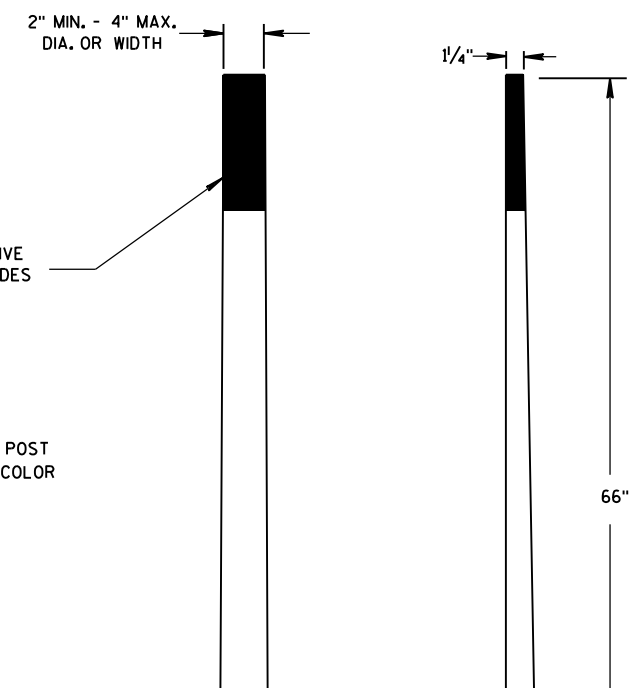
FRONT VIEW SIDE VIEW  
ALTERNATE 1



SECTION A-A

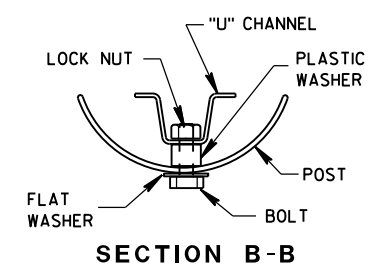


FRONT VIEW SIDE VIEW  
ALTERNATE 1

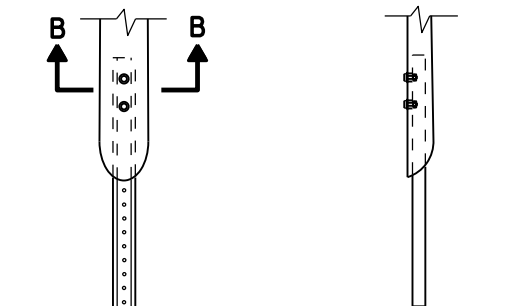


FRONT VIEW SIDE VIEW  
ALTERNATE 2

**FLEXIBLE MARKER POSTS**

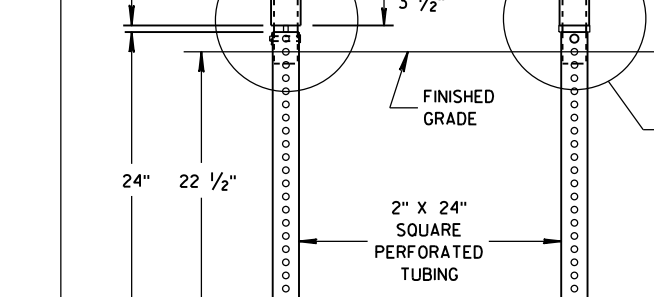
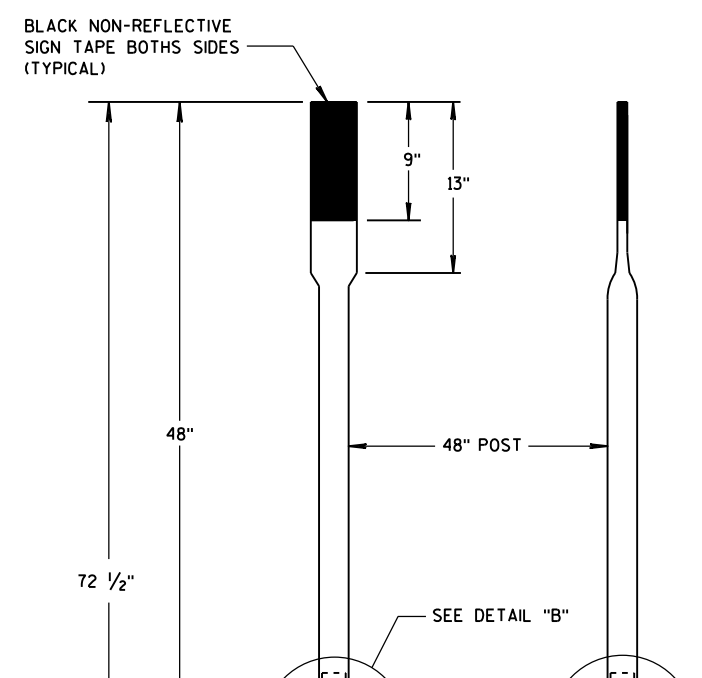


SECTION B-B

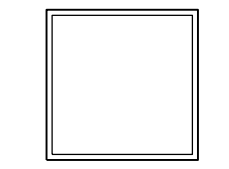


FRONT VIEW SIDE VIEW  
ALTERNATE 2

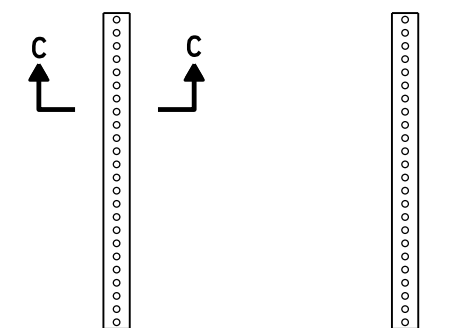
**FLEXIBLE MARKER POST ANCHORS**



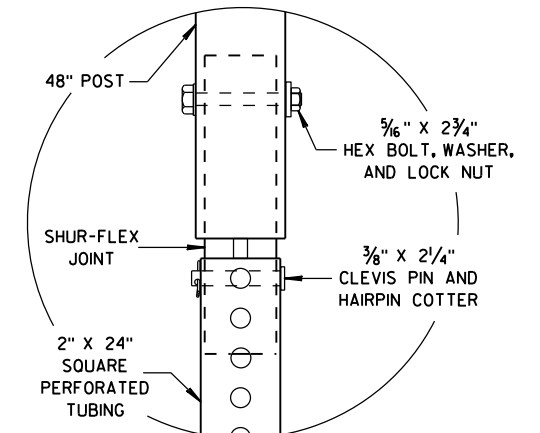
FRONT VIEW SIDE VIEW  
ALTERNATE 3



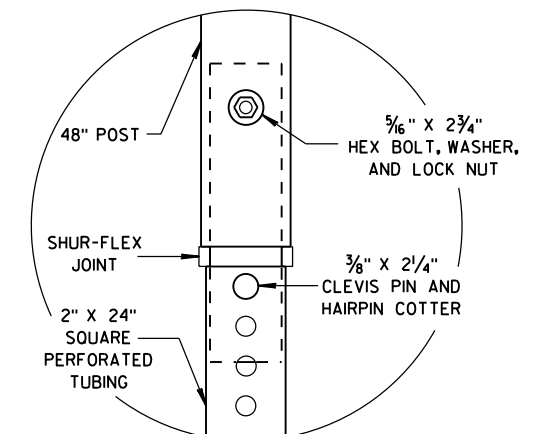
SECTION C-C



FRONT VIEW SIDE VIEW  
ALTERNATE 3



DETAIL B

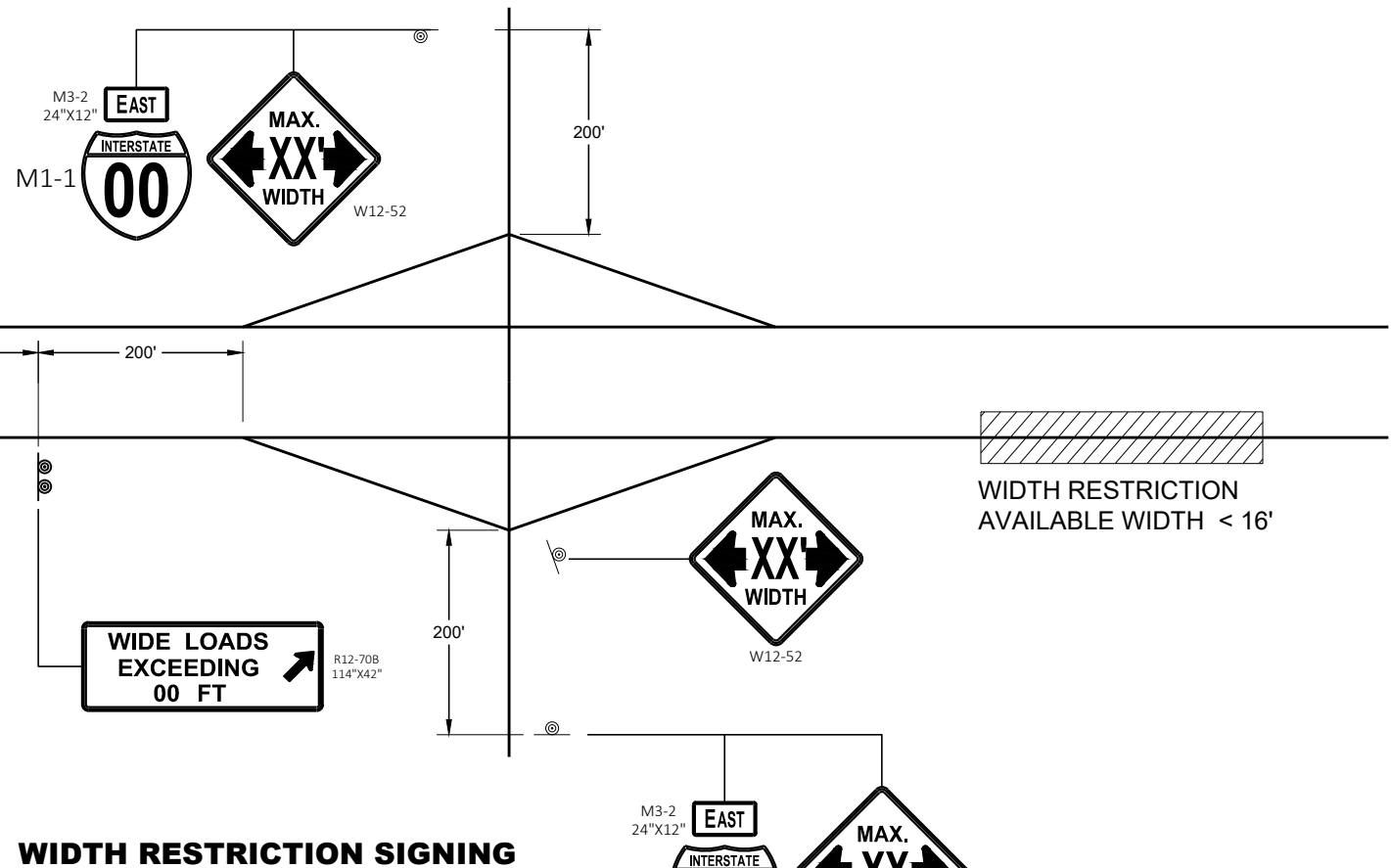


DETAIL C

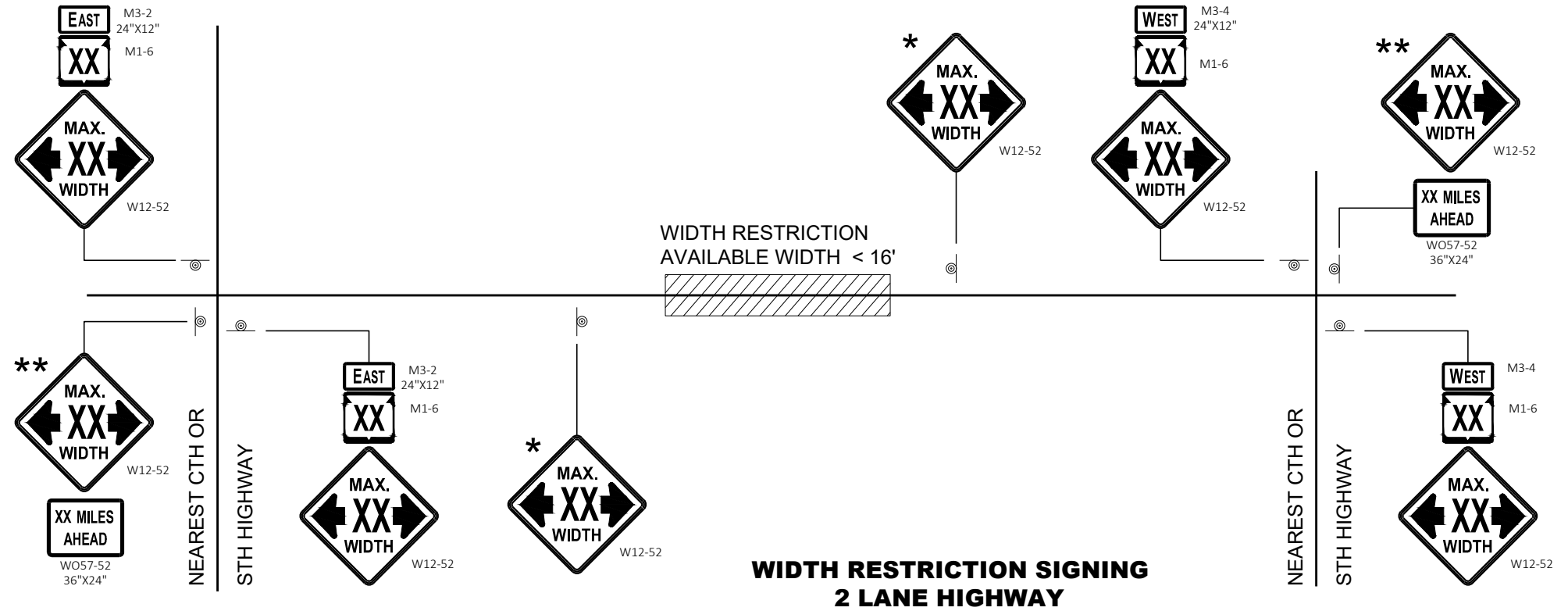
**FLEXIBLE MARKER POST FOR CULVERT END**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
10/1/2012 DATE /S/ Travis Feltes  
STATE TRAFFIC ENGINEER OF DESIGN  
FHWA



**WIDTH RESTRICTION SIGNING**



**WIDTH RESTRICTION SIGNING  
2 LANE HIGHWAY**

**LEGEND**

⊙ SIGN ON PERMANENT SUPPORT

**GENERAL NOTES**

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

WIDTH ON SIGN TO BE APPROXIMATELY ONE FOOT LESS THAN AVAILABLE WIDTH.

\* PLACE 500 FEET AFTER THE W20 - 1A AND 500 FEET BEFORE ADDITIONAL SIGNS FOR ROADWAYS WITH A PRE - CONSTRUCTION SPEED LIMIT OF 45 MPH OR MORE. FOR 35-40 MPH, USE 350 FOOT TYPICAL SPACING. FOR 25-30 MPH, USE 200 FOOT TYPICAL SPACING.

\*\* SIGN SHALL BE VISIBLE FROM ROADWAY.

\*\*\* ADDITIONAL SIGNS NEEDED IF THERE IS AN ON RAMP BETWEEN SIGNS.

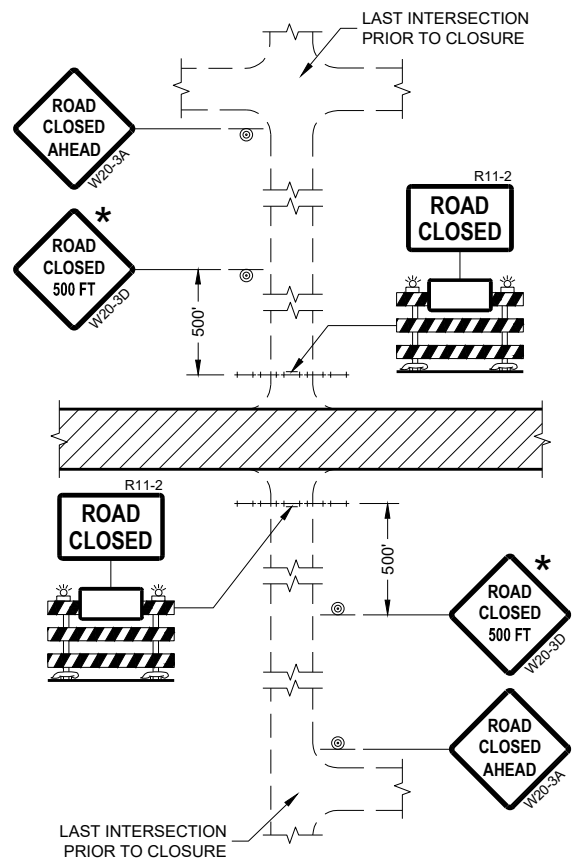


WIDTH ON SIGN TO BE APPROX. 1 - FOOT LESS THAN AVAILABLE WIDTH

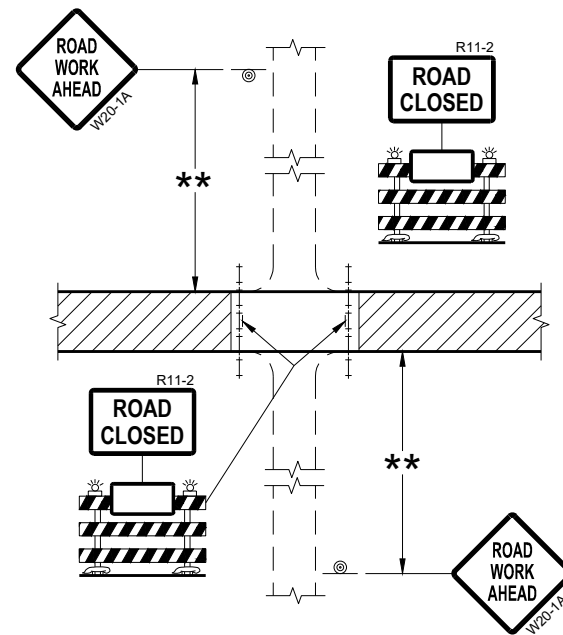
**ADVANCED WIDTH RESTRICTION SIGNING**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

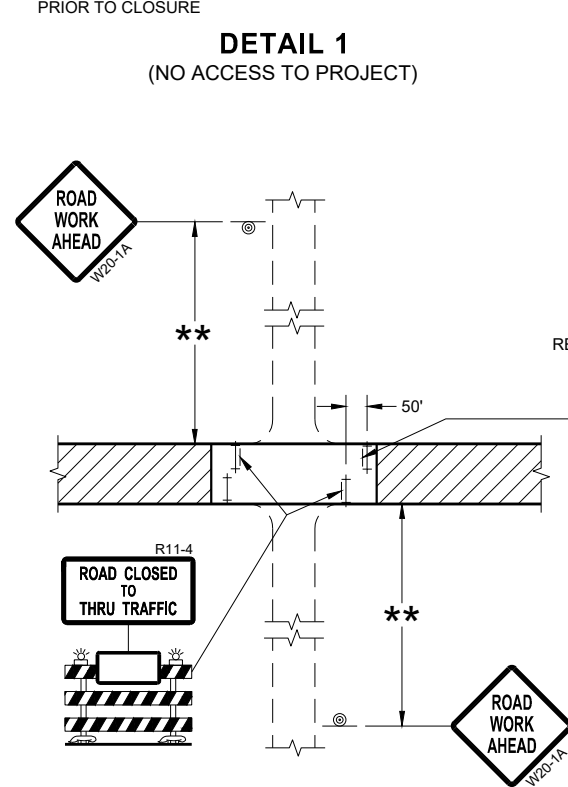
APPROVED  
February 2020 /S/ Andrew Heidtke  
DATE WORK ZONE ENGINEER  
FHWA



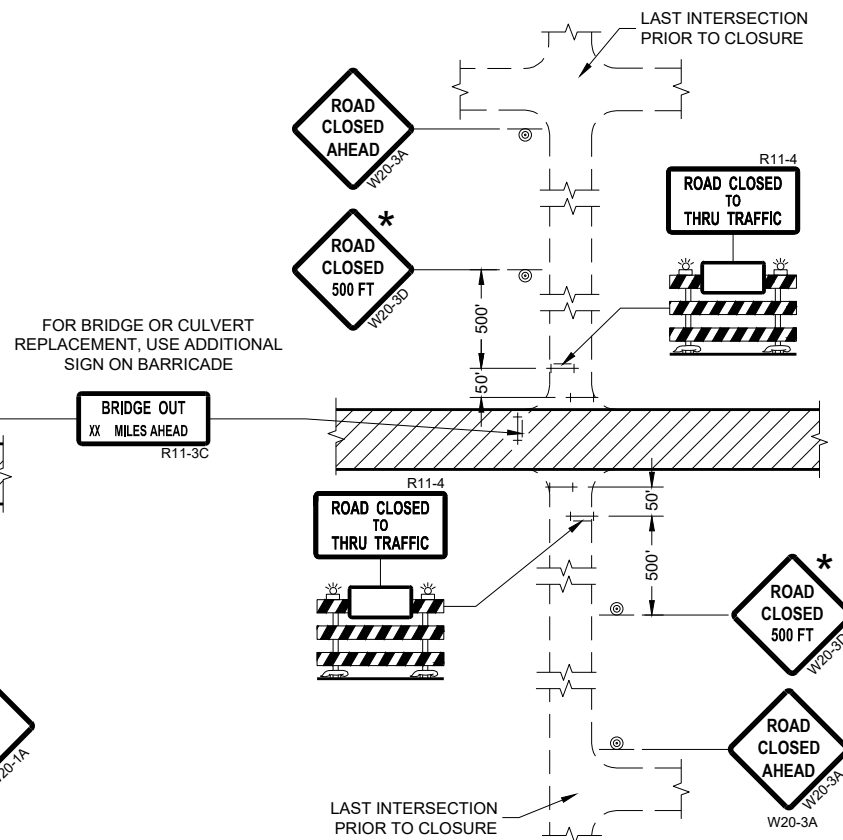
**DETAIL 1**  
(NO ACCESS TO PROJECT)



**DETAIL 2**  
(PUBLIC CROSS-TRAFFIC MAINTAINED.  
NO ACCESS TO PROJECT)



**DETAIL 3**  
(PUBLIC CROSS-TRAFFIC MAINTAINED.  
CONTRACTOR, LOCAL BUSINESS AND  
RESIDENT ACCESS TO PROJECT)



**DETAIL 4**  
(CONTRACTOR, LOCAL BUSINESS AND  
RESIDENT ACCESS TO PROJECT)

**GENERAL NOTES**

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE (500 FEET DESIRABLE) TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3, AND R11-4 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:  
R11-2 SHALL BE 48" X 30".  
R11-4 AND R11-3 SHALL BE 60" X 30".

\* OMIT THE "ROAD CLOSED 500 FT." SIGN IF THE LAST INTERSECTION IS 500 FEET OR LESS FROM THE WORK ZONE.

\*\* 500' MAX. OR AT LAST INTERSECTION, WHICHEVER IS CLOSEST.

**LEGEND**

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA

**BARRICADES AND SIGNS  
FOR  
SIDEROAD CLOSURES**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
July 2018 /S/ Andrew Heidtke  
DATE WORK ZONE ENGINEER

**GENERAL NOTES**

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.




ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

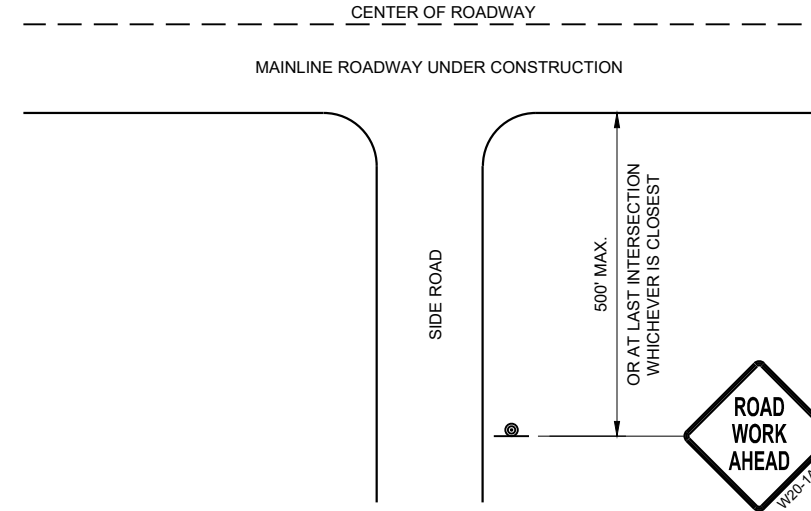
SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

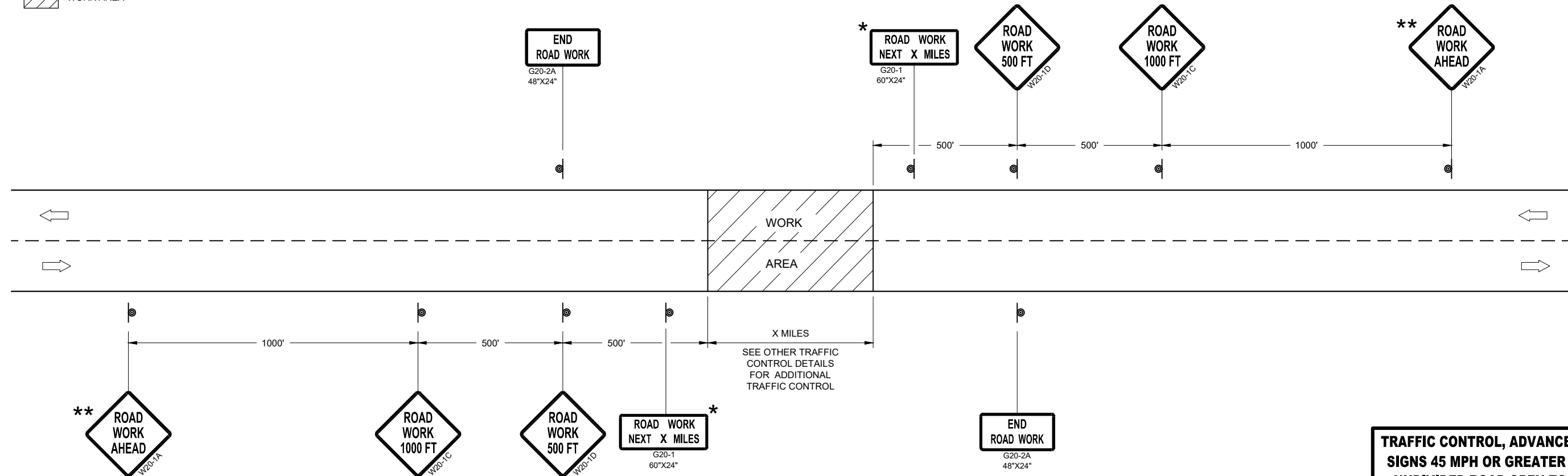
- \* OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS
- \*\* PLACE AN ADDITIONAL W20-1A "ROAD WORK AHEAD" SIGN IF WORK AREA WITHIN THE PROJECT IS SEPARATED BY MORE THAN 2 MILES FROM PREVIOUS WORK AREA.

**LEGEND**

-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC
-  WORK AREA



**TYPICAL SIDE ROAD APPROACH  
WARNING SIGN DETAIL**



**TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45MPH OR GREATER**

**TRAFFIC CONTROL, ADVANCE WARNING  
SIGNS 45 MPH OR GREATER TWO-WAY  
UNDIVIDED ROAD OPEN TO TRAFFIC**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
DATE July 2018 /S/ Andrew Heidtke  
WORK ZONE ENGINEER

FHWA

**GENERAL NOTES**

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.




ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS, 36"X36" SIGNS MAY BE USED INSTEAD OF 48" X 48" SIGNS.

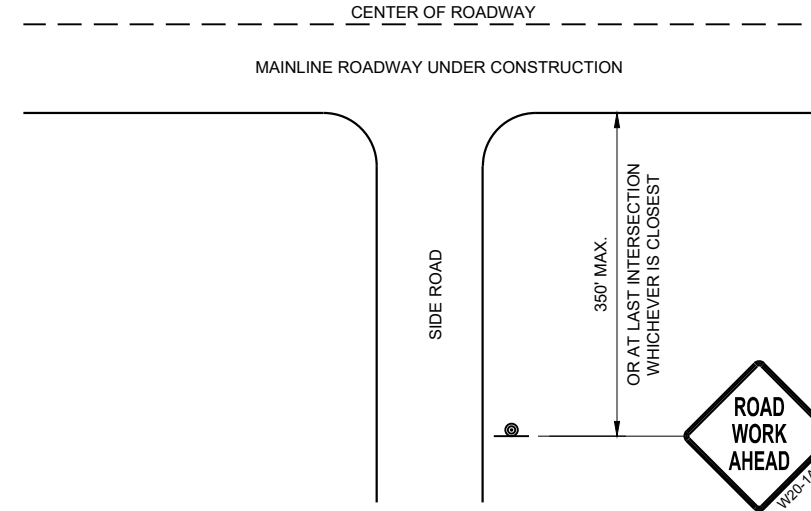
SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

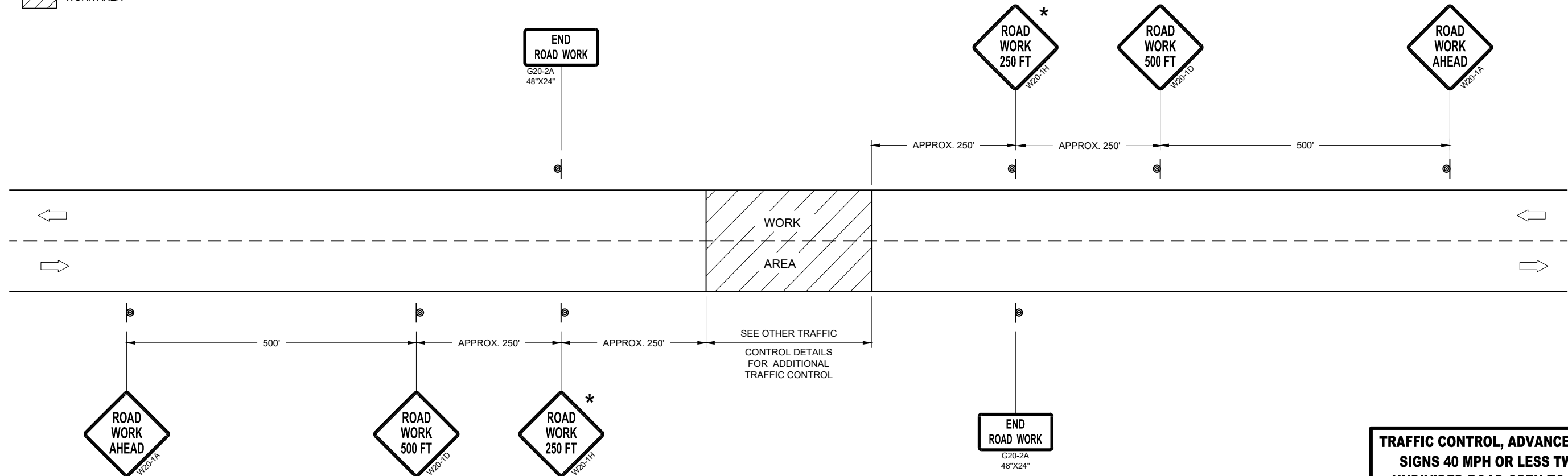
\* THE THIRD W20-1 SIGN IS REQUIRED ONLY IF THERE IS AN INTERSECTION BETWEEN THE "ROAD WORK 500 FEET" SIGN AND THE WORK ZONE. ADJUST THE PLACEMENT OF THIS SIGN BASED ON INTERSECTION LOCATION AND OTHER FIELD CONDITIONS.

**LEGEND**

-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC
-  WORK AREA



**TYPICAL SIDE ROAD APPROACH  
WARNING SIGN DETAIL**



**TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40MPH OR LESS**

**TRAFFIC CONTROL, ADVANCE WARNING  
SIGNS 40 MPH OR LESS TWO-WAY  
UNDIVIDED ROAD OPEN TO TRAFFIC**

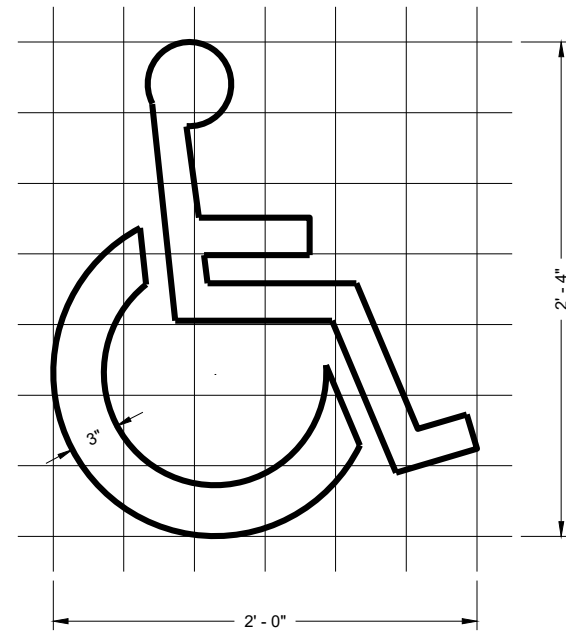
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
July 2018 /S/ Andrew Heidtke  
DATE WORK ZONE ENGINEER

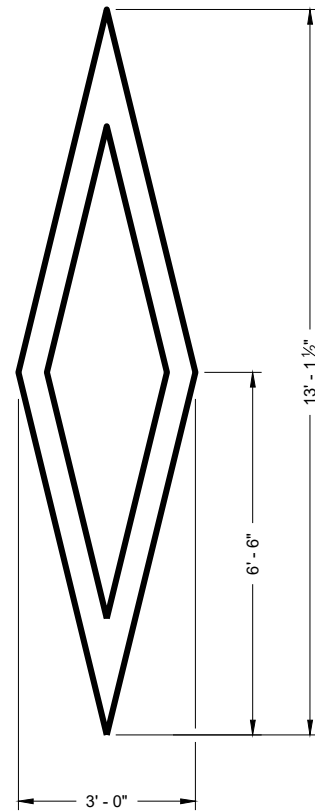
FHWA

**GENERAL NOTES**

DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

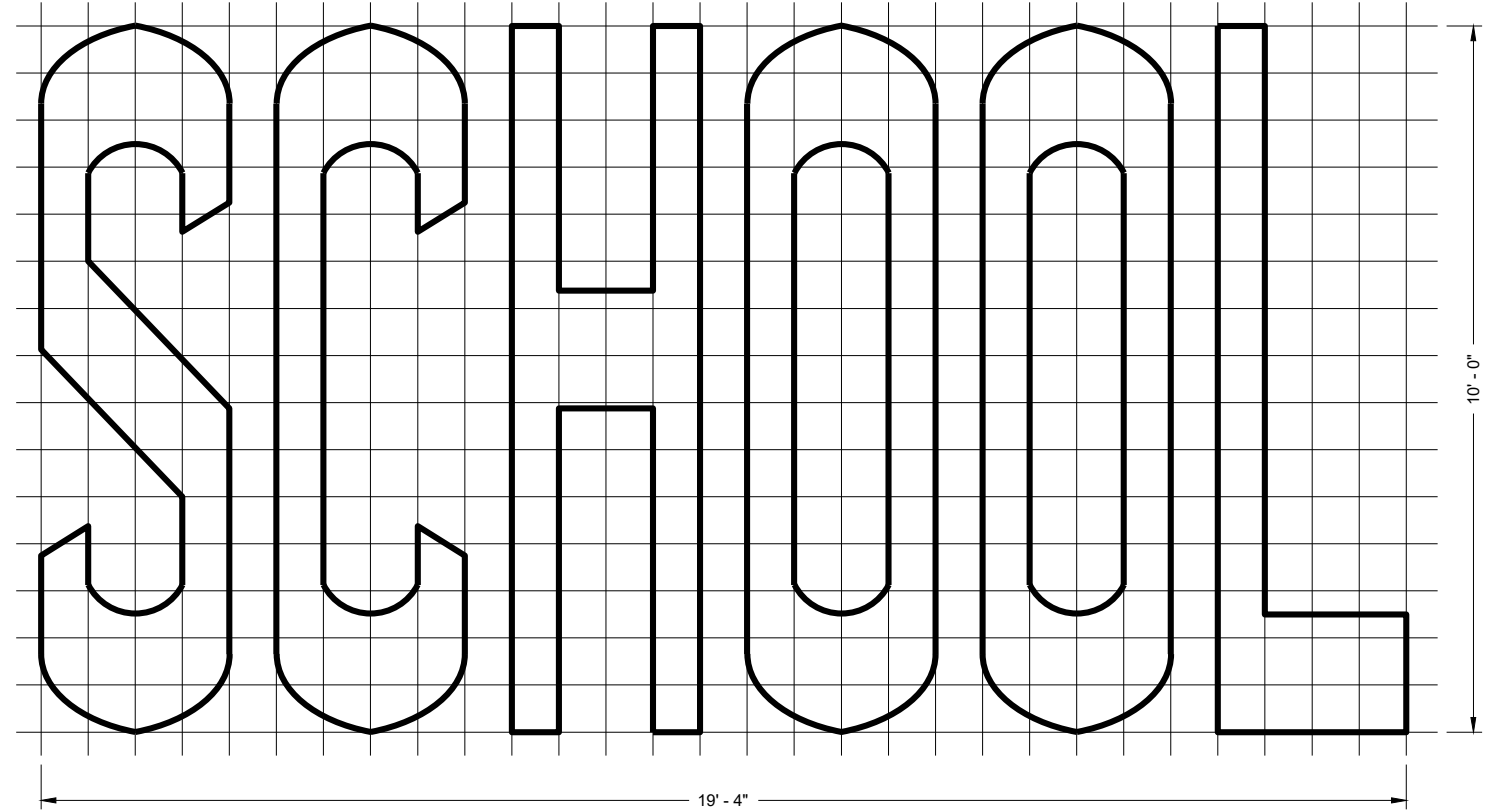
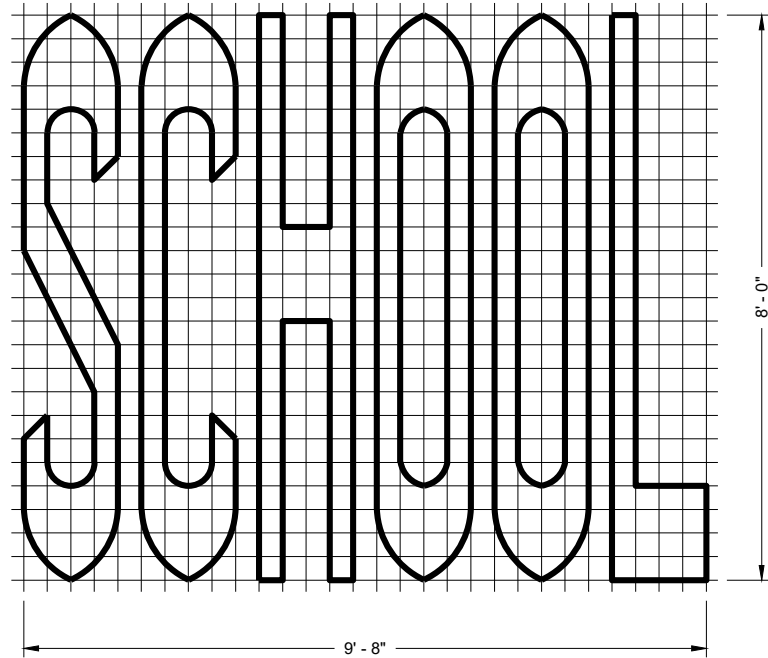
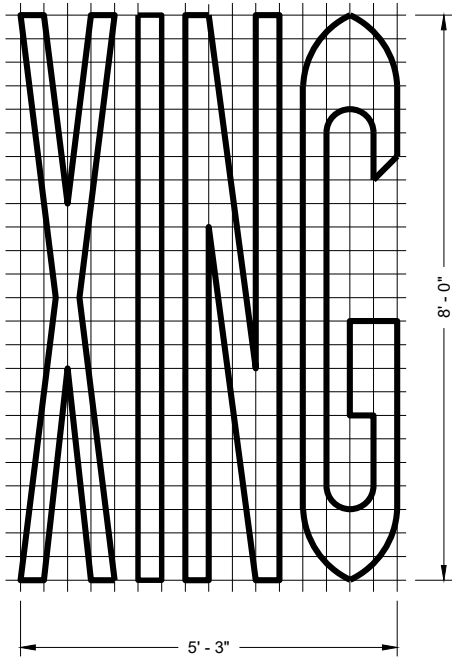
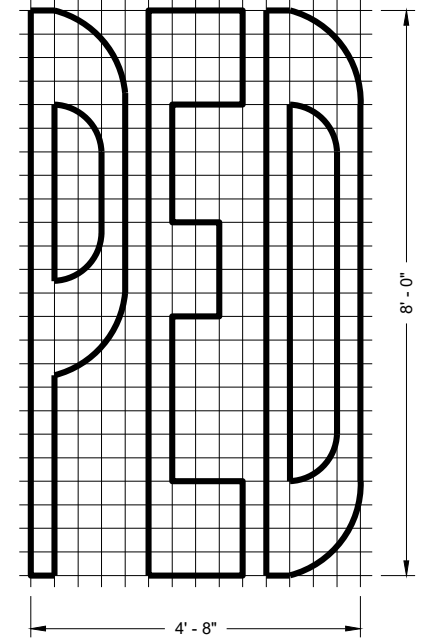
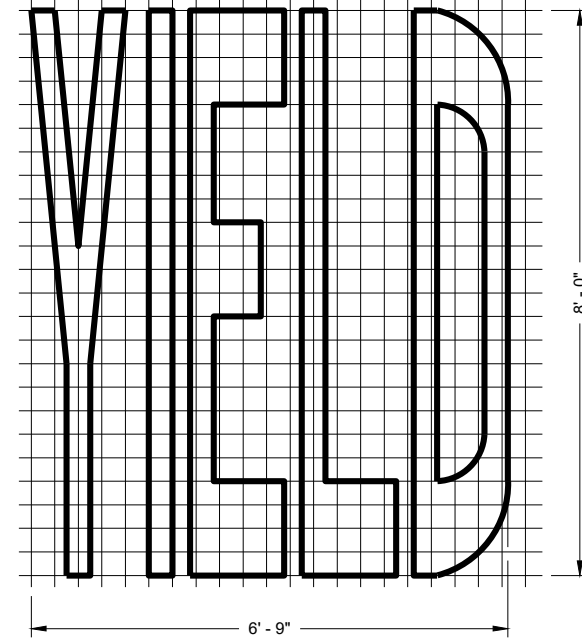
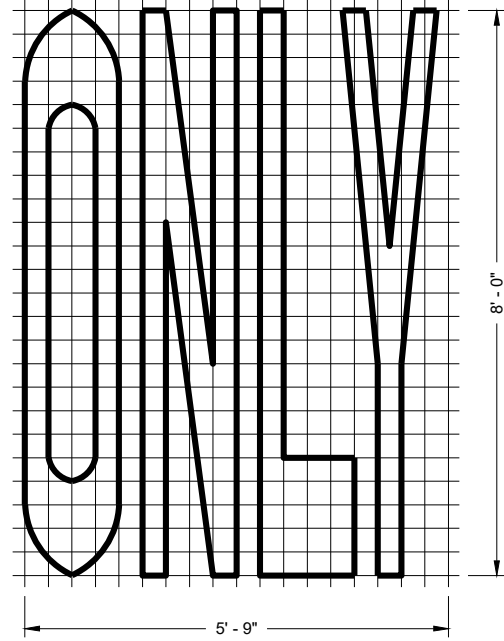
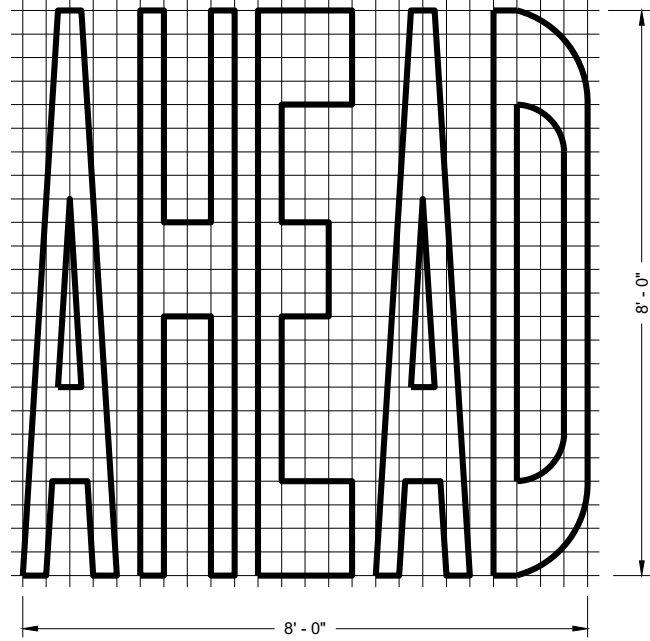
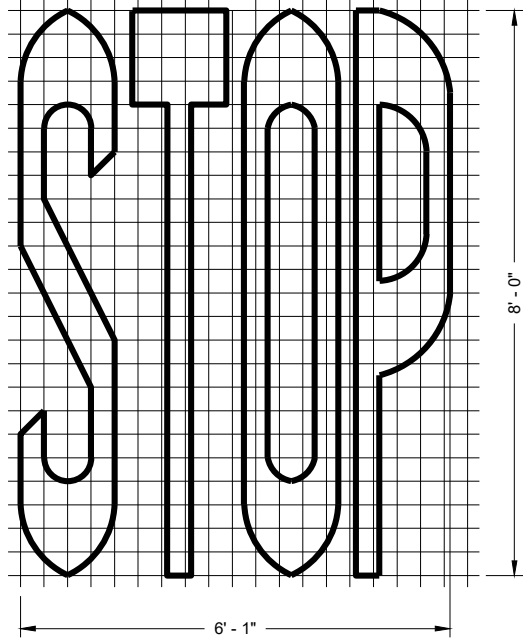


**HANDICAP SYMBOL**



**PREFERENTIAL LANE SYMBOL**

<b>PAVEMENT MARKING SYMBOLS</b>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED November 2019 DATE	/S/ Matthew Rauch STATE SIGNING AND MARKING ENGINEER
FHWA	



SINGLE LANE

TWO - LANE

**GENERAL NOTES**

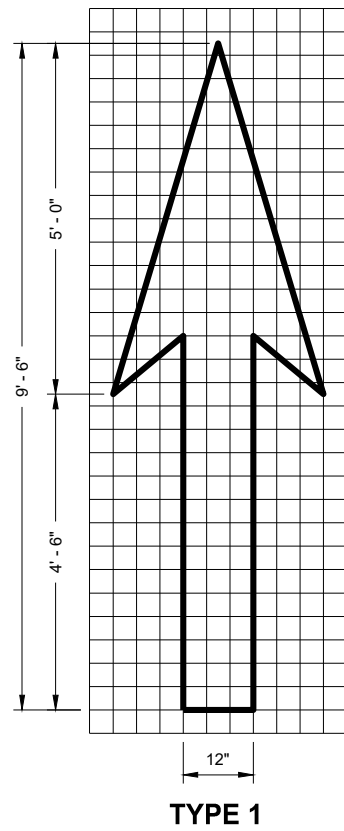
DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

**PAVEMENT MARKING WORDS**

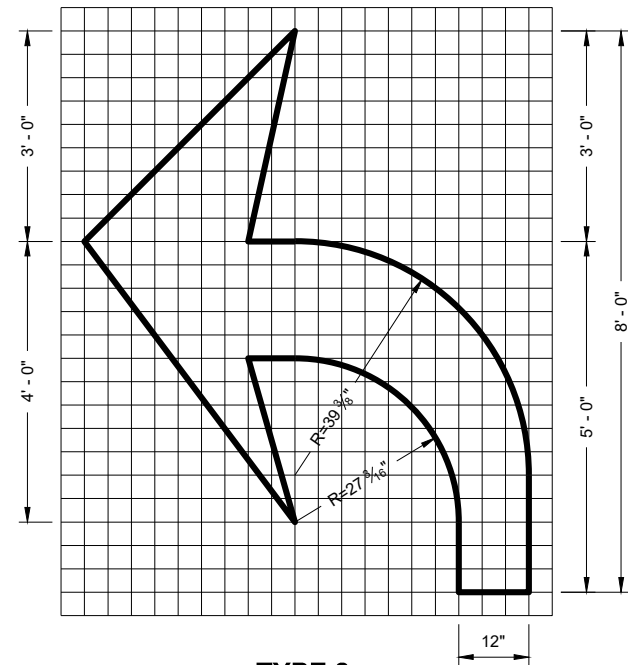
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
November 2019 /S/ Matthew Rauch  
DATE STATE SIGNING AND MARKING  
ENGINEER

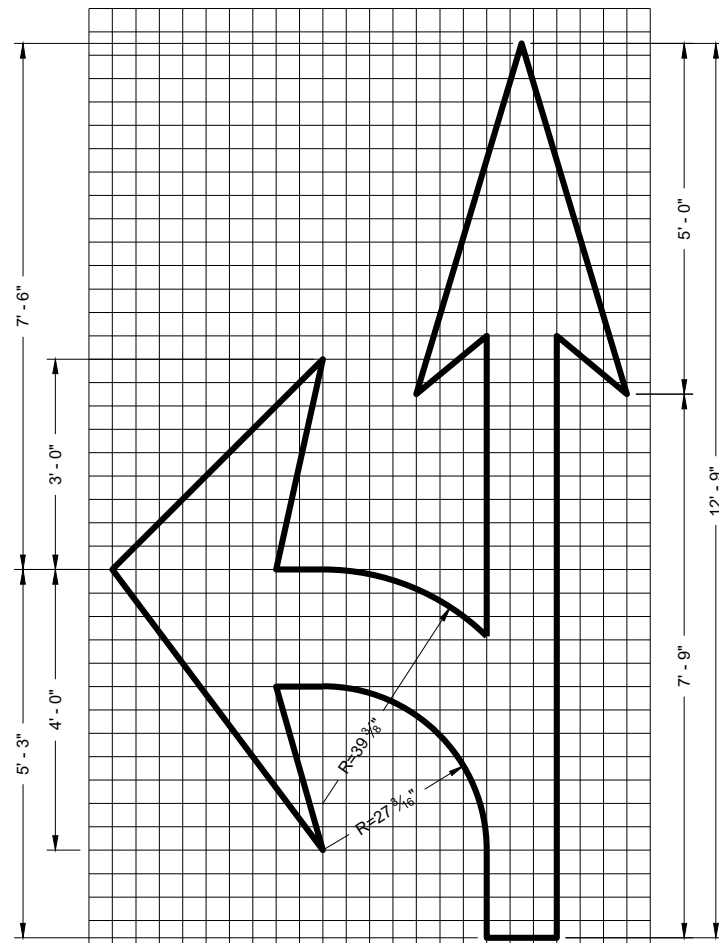
FHWA



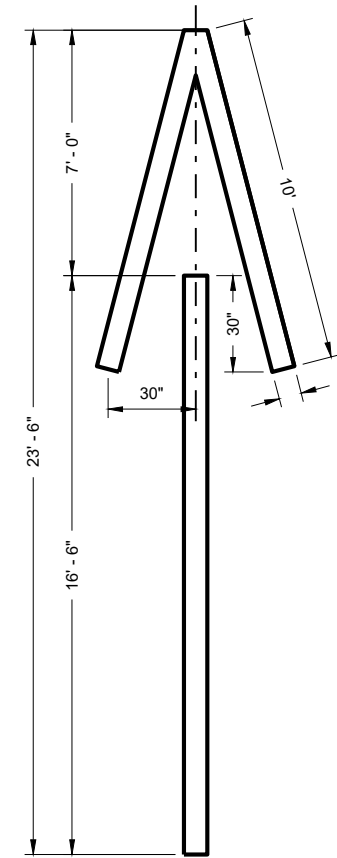
TYPE 1



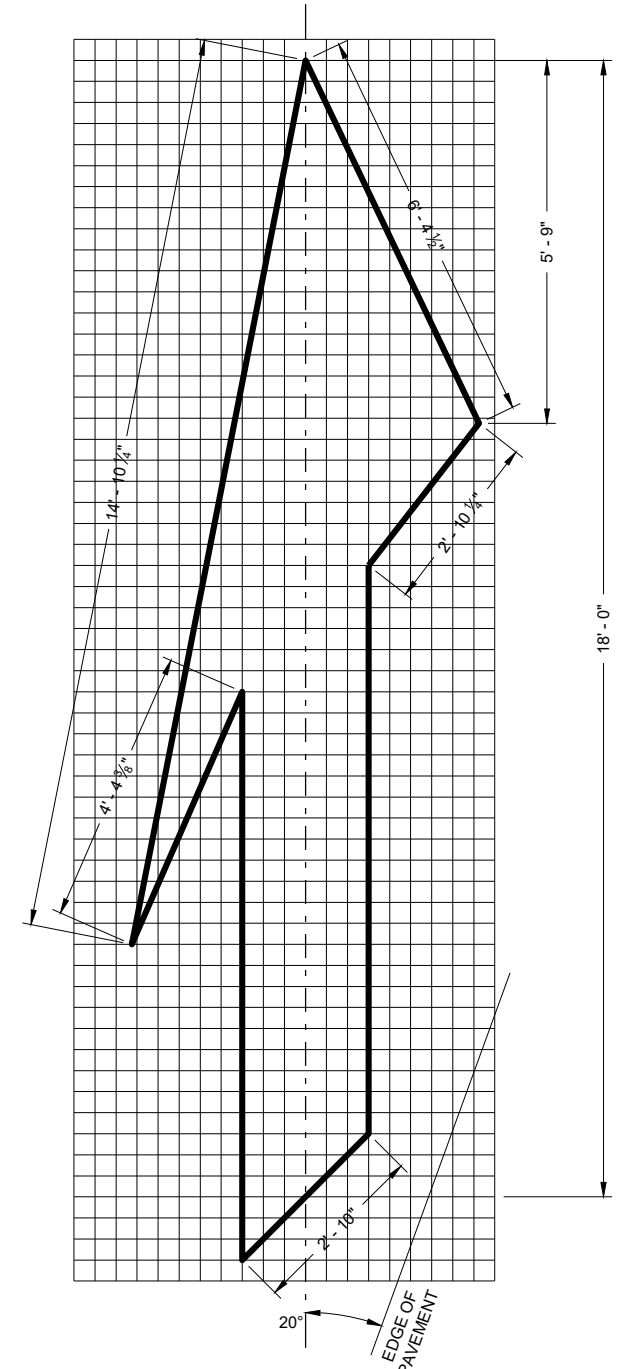
TYPE 2



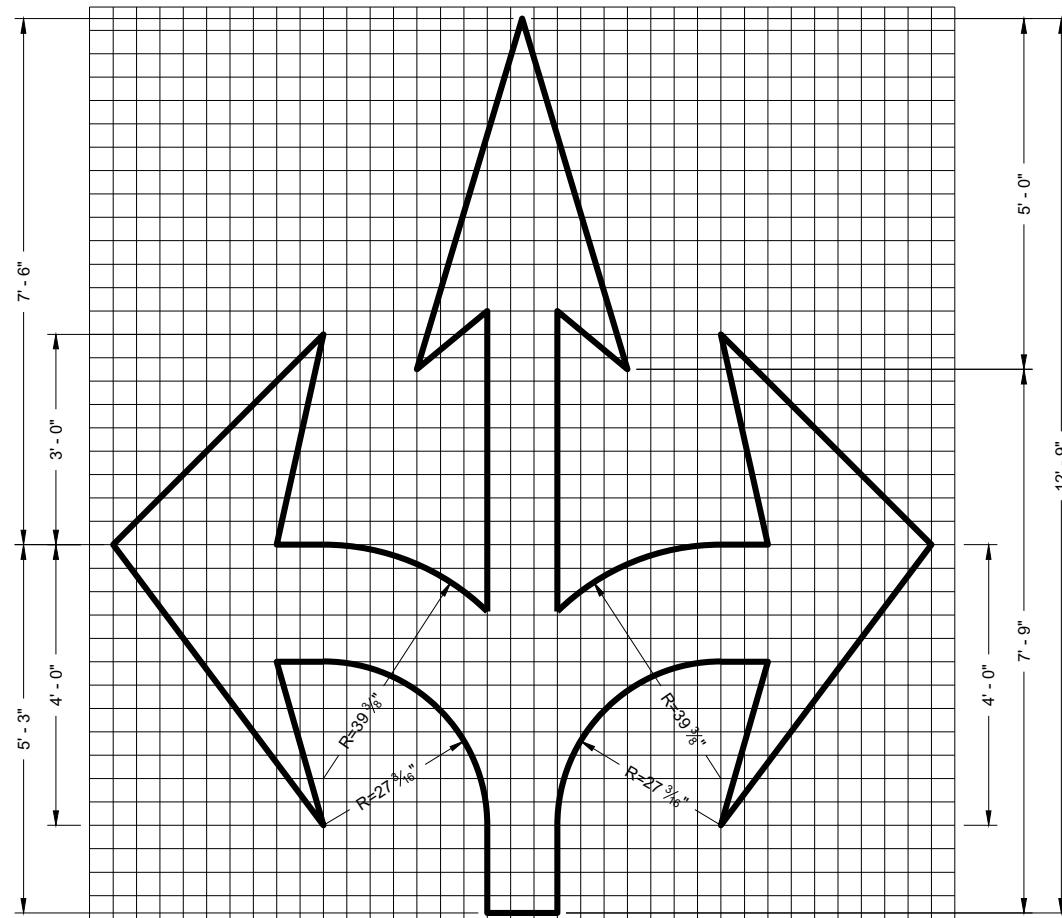
TYPE 3



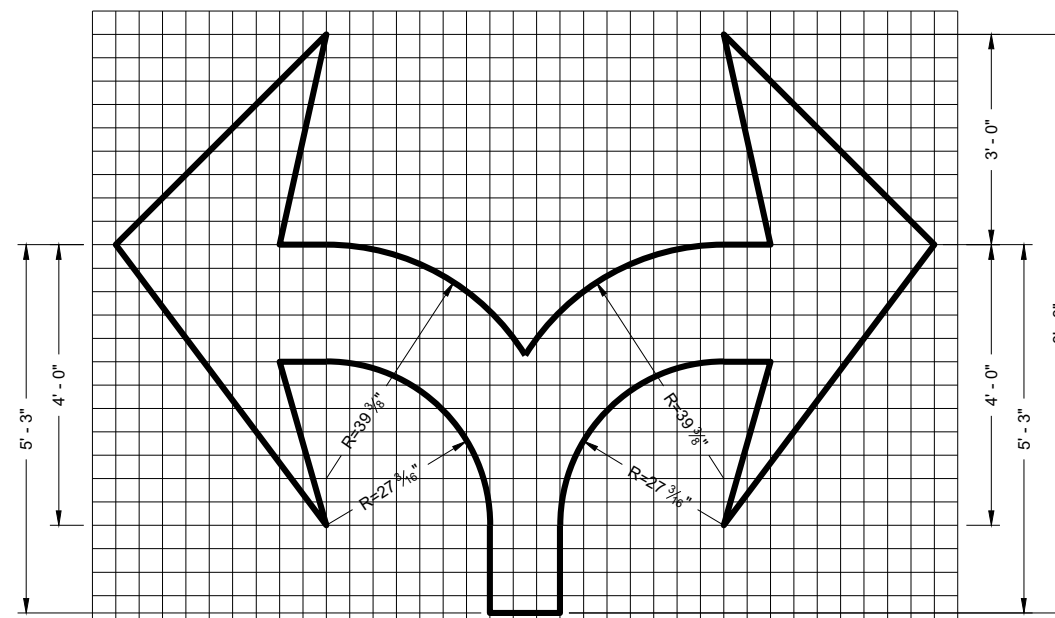
TYPE 4



TYPE 5 LANE DROP ARROW



TYPE 6



TYPE 7

GENERAL NOTES

DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

PAVEMENT MARKING ARROWS

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED	/s/ Matthew Rauch
November 2019	STATE SIGNING AND MARKING ENGINEER
DATE	

FHWA






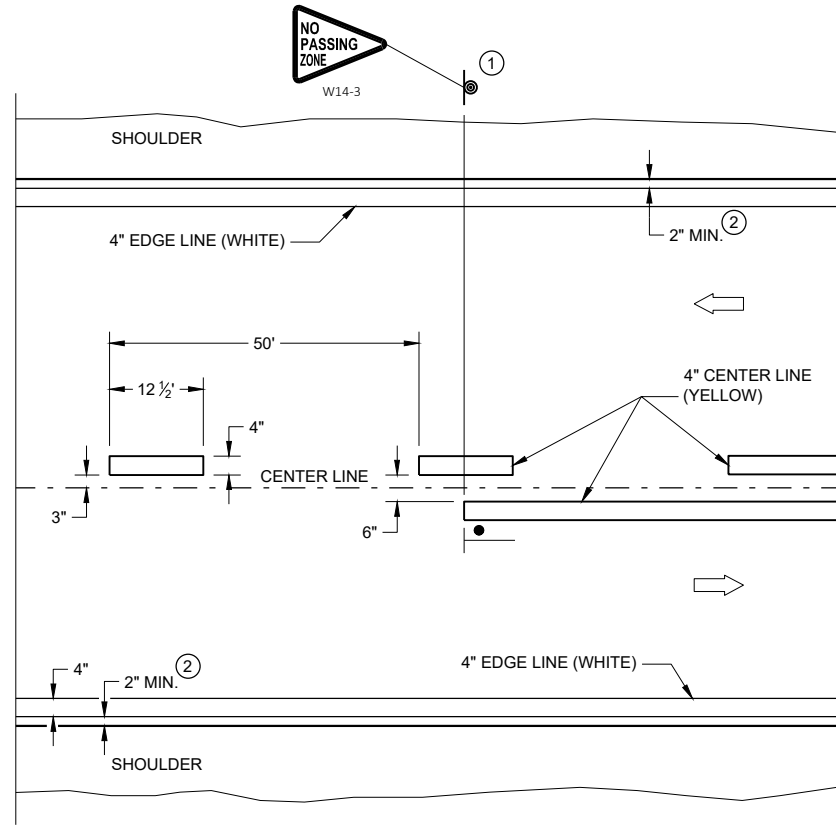
**GENERAL NOTES**

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

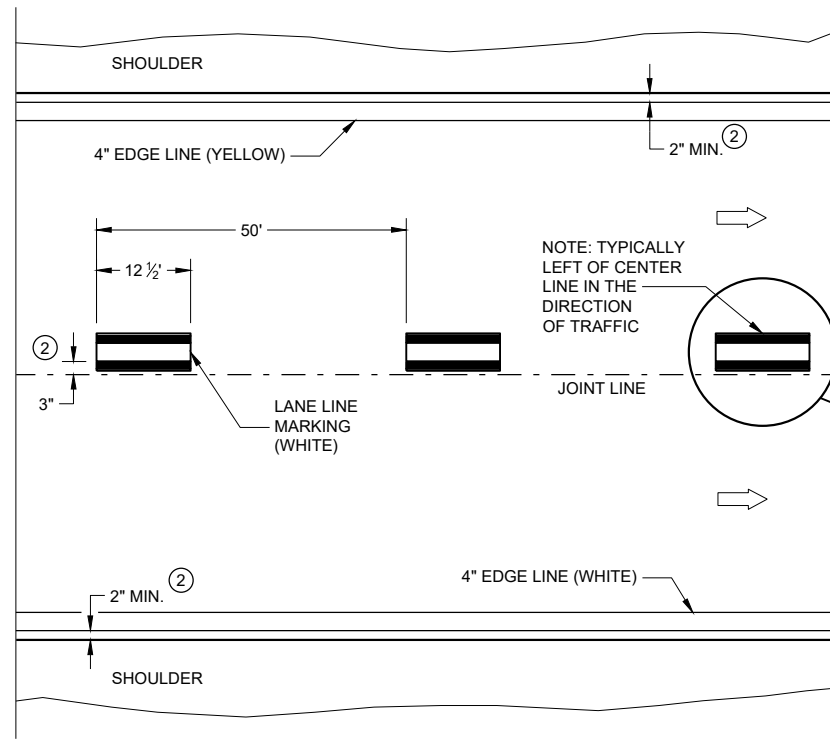
- ① LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING
- ② MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

**LEGEND**

-  "T" MARKING
-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC

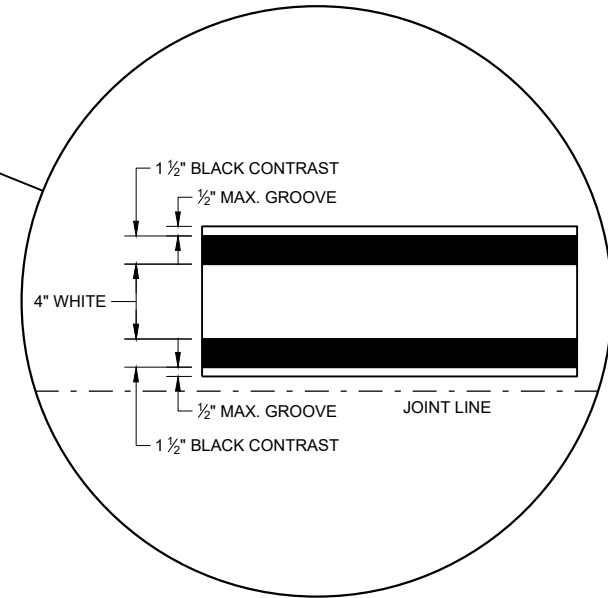


**TWO WAY TRAFFIC**



**ONE WAY TRAFFIC**

**PERMANENT PAVEMENT MARKING**



6

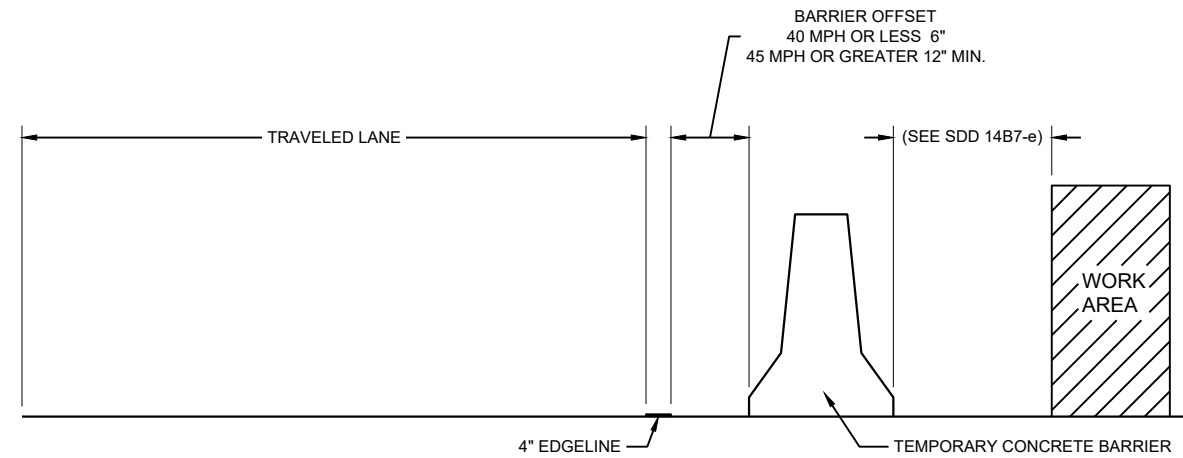
6

**PERMANENT LONGITUDINAL PAVEMENT MARKINGS**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
DATE: May 2022 /S/ Jeannie Silver  
STATEWIDE SIGNING AND MARKING ENGINEER

FHWA



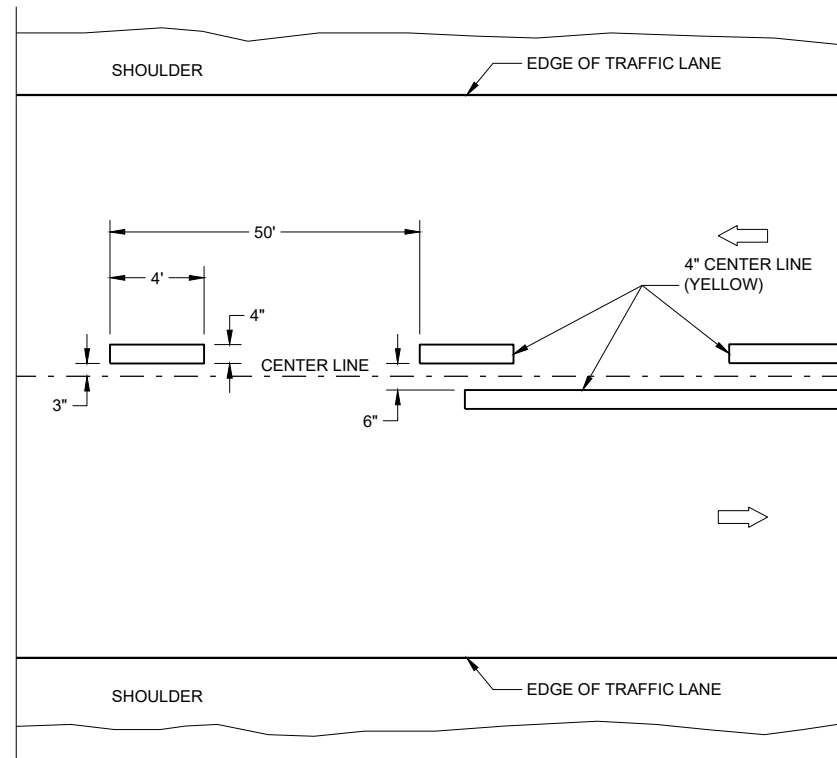
**TEMPORARY BARRIER OFFSET FROM EDGELINE**

**GENERAL NOTES**

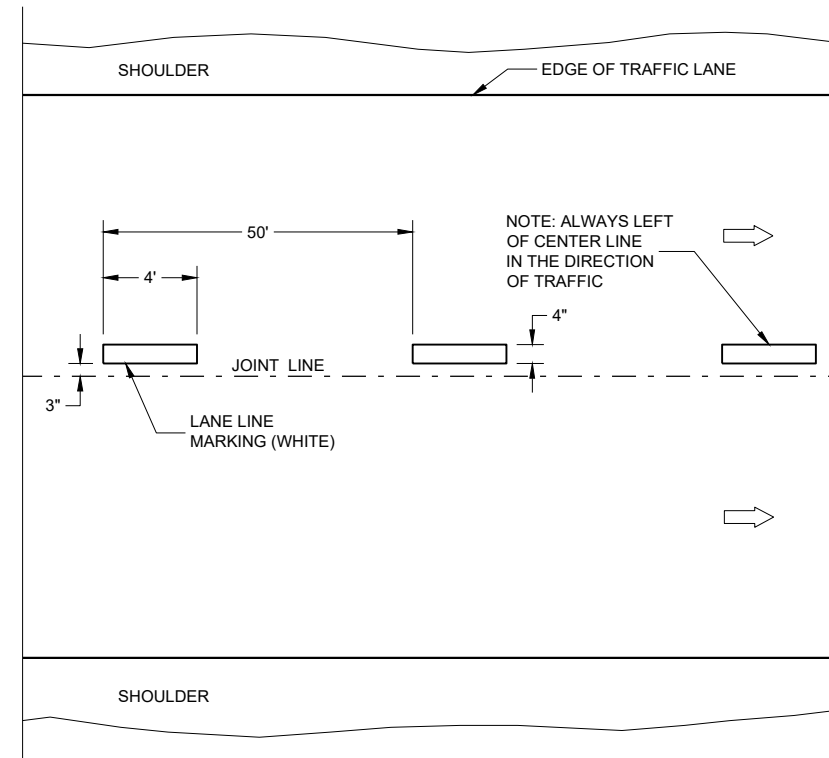
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

**LEGEND**

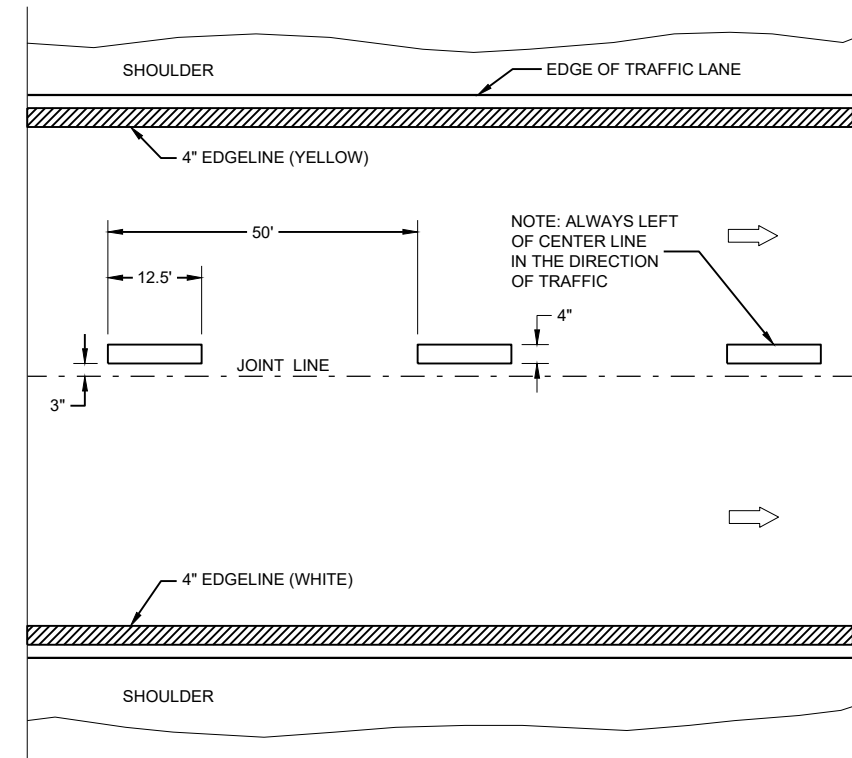
➔ DIRECTION OF TRAFFIC



**TWO WAY TRAFFIC**



**ONE WAY TRAFFIC**



**FREEWAYS AND EXPRESSWAYS**

**TEMPORARY PAVEMENT MARKING**

**TEMPORARY LONGITUDINAL PAVEMENT MARKING**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

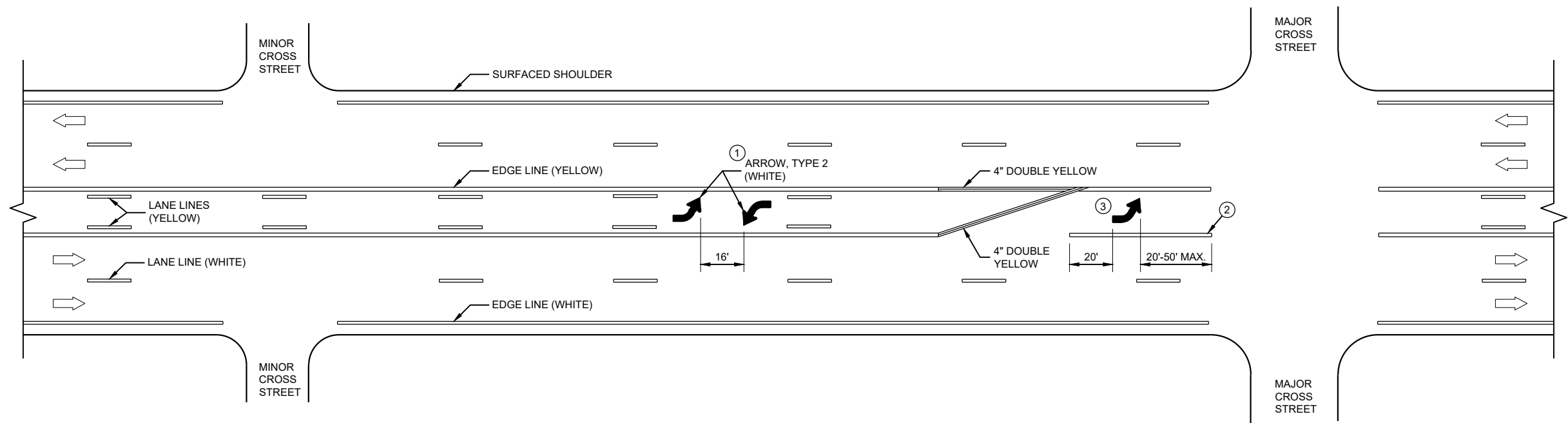
APPROVED  
May 2022 DATE /S/ Jeannie Silver  
STATEWIDE SIGNING AND MARKING ENGINEER

FHWA

**GENERAL NOTES**

- ① A SET OF ARROWS IS REQUIRED EVERY 400 FEET OR NEAR INTERSECTIONS OR DRIVEWAYS WITH TURNING TRAFFIC.
- ② 8" WHITE
- ③ TURN BAY LENGTH OF LESS THAN 48' DOES NOT REQUIRE PAVEMENT ARROWS OR TEXT.

➡ DIRECTION OF TRAFFIC



**TWO WAY LEFT TURN LANE**

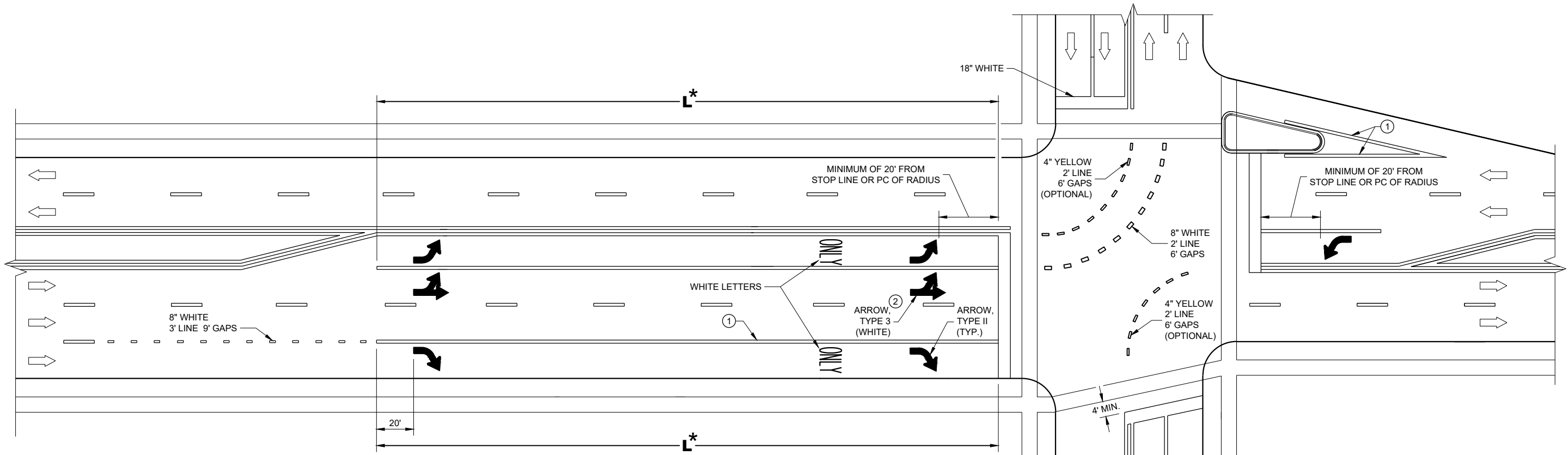
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6

SDD 15C08 - 22c

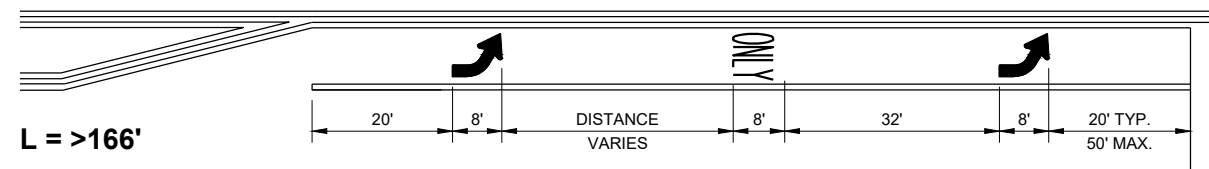
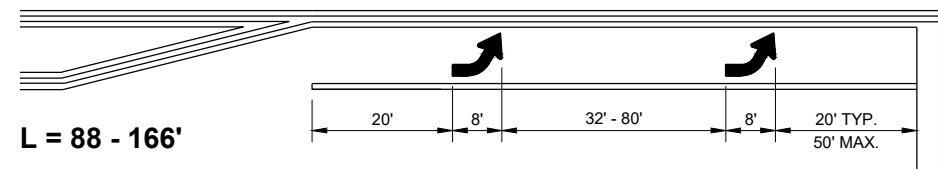
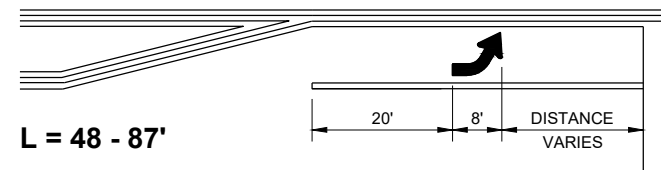
SDD 15C08 - 22c

<p><b>PAVEMENT MARKING (TURN LANES)</b></p>
<p>STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION</p>



**TURN LANE OPTIONS**

LENGTH OF TURN BAY ( **L** ) OF 0 - 47' DOES NOT REQUIRE PAVEMENT MARKING ARROWS OR WORDS



\*(SEE TURN LANE OPTIONS FOR PLACEMENT OF PAVEMENT MARKING ARROWS AND WORDS)

**GENERAL NOTES**

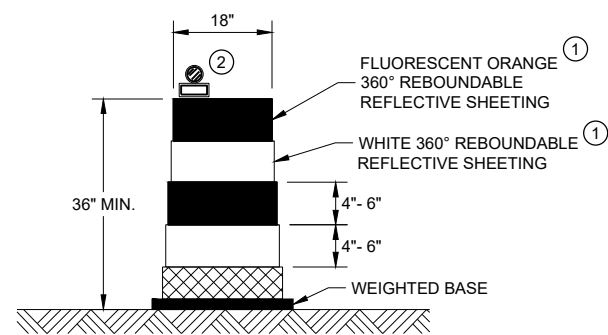
- ① 8" WHITE
- ② QUANTITY AND LOCATION OF TYPE 3 ARROWS ARE THE SAME AS THE TYPE II ARROWS IN THE ADJACENT TURN LANE. FOR TURN LANES WITH A PHYSICAL SEPARATION IN THE SAME DIRECTION OF TRAVEL, THE ARROWS AND "ONLY" MARKING MAY BE ELIMINATED.

➡ DIRECTION OF TRAFFIC

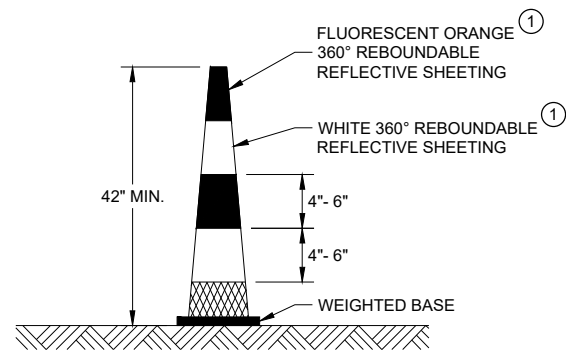
**L** = LENGTH OF TURN BAY

**PAVEMENT MARKING (TURN LANES)**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

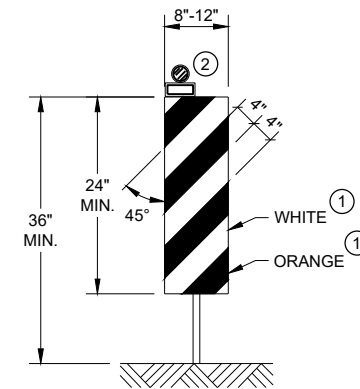


**DRUM**



**42" CONE**

DO NOT USE IN TAPERS  
 1/2 SPACING OF DRUMS

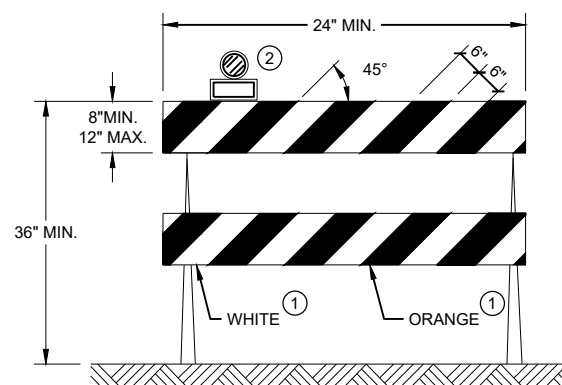


**VERTICAL PANEL**

THE STRIPES SHALL SLOPE DOWNWARD TO THE TRAFFIC SIDE FOR CHANNELIZATION.

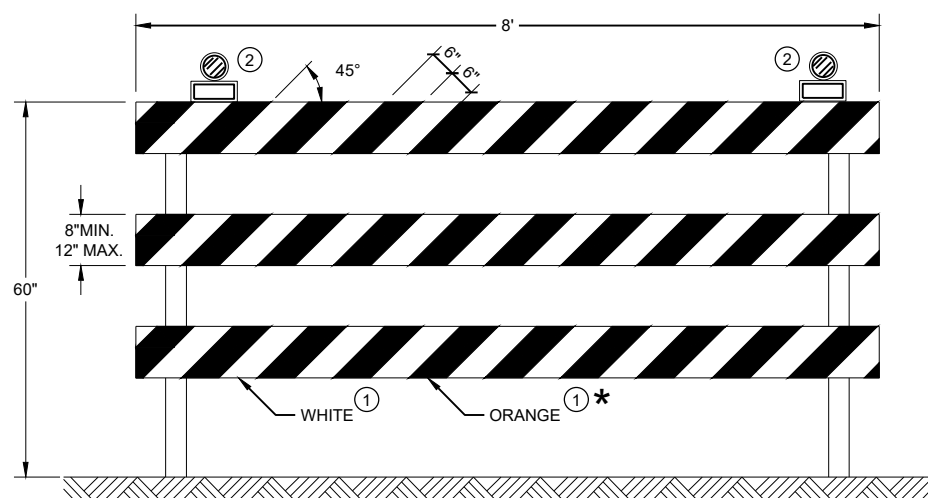
**GENERAL NOTES**

- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.



**TYPE II BARRICADE**

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES MAY BE USED. ALL STRIPES SHALL SLOPE DOWNWARD TO THE TRAFFIC SIDE FOR CHANNELIZATION.








**TYPE III BARRICADE**

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.

\* IF USED FOR A PERMANENT APPLICATION USE RED SHEETING.

<b>CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS</b>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2021 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

**LEGEND**

-  SIGN ON PORTABLE OR PERMANENT SUPPORT
-  TEMPORARY PORTABLE RUMBLE STRIP ARRAY
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

**GENERAL NOTES**

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS, DEVICES, AND LOCATION OF ALL FLAGGERS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

**FLAGGING**

FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT REMOVE TEMPORARY PORTABLE RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.

- ① FOR MOVING WORK OPERATIONS, POST ADDITIONAL W20-7A FLAGGER SIGNS AT APPROXIMATELY 3,500' INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.
- ② SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

WHEN THE DISTANCE BETWEEN FLAGGERS EXCEEDS 2 MILES, A PILOT CAR IS REQUIRED. WHEN CURVES REDUCE SIGHT DISTANCE BELOW 400', A PILOT CAR IS REQUIRED.

**TEMPORARY PORTABLE RUMBLE STRIPS**

UTILIZE TEMPORARY PORTABLE RUMBLE STRIPS ON ALL FLAGGING OPERATIONS.

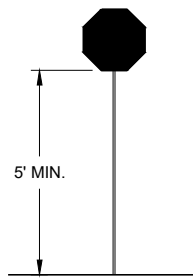
- ③ EACH TEMPORARY PORTABLE RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS PLACED TRANSVERSE ACROSS THE LANE AT THE LOCATIONS SHOWN. WITHIN EACH ARRAY, SPACING BETWEEN RUMBLE STRIPS SHALL BE 15 FEET ON CENTER

ONLY USE TEMPORARY PORTABLE RUMBLE STRIPS FROM THE APPROVED PRODUCTS LIST.

INSTALL TEMPORARY RUMBLE STRIPS PER MANUFACTURER'S RECOMMENDATIONS.

PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.

DO NOT INSTALL TEMPORARY PORTABLE RUMBLE STRIPS ON GRAVEL, MILLED SURFACES, OR ASPHALT THAT HAS BEEN PAVED LESS THAN 12 HOURS.



**STOP/SLOW PADDLE ON SUPPORT STAFF**

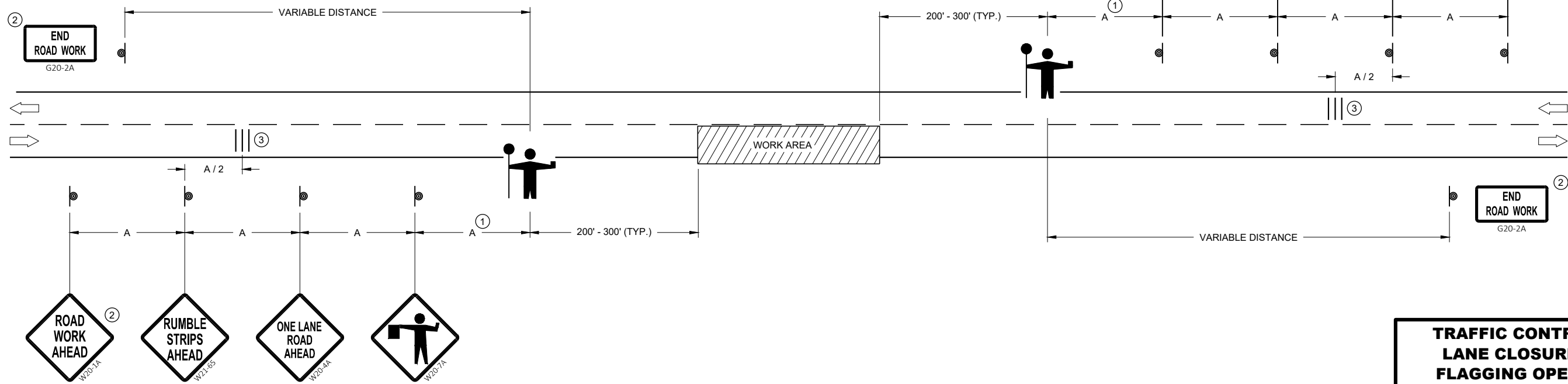
**SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE**

SPEED LIMIT	SPACING "A"
25-30 MPH	200'
35-40 MPH	350'
45-55 MPH	500'



W03-4

USE OF W03-4 SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7A AND W20-4A SIGNS, USING SPACING "A".










**TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
DATE: May 2022 /S/ Andrew Heidtke  
WORK ZONE ENGINEER

FHWA

**GENERAL NOTES**

-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL CONE 42-INCH
-  TRAFFIC CONTROL DRUM
-  TEMPORARY PORTABLE RUMBLE STRIP ARRAY
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  **AFAD** AUTOMATED FLAGGER ASSISTANCE DEVICE (AFAD)

**GENERAL NOTES**

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS, DEVICES, AND LOCATION OF ALL FLAGGERS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

**FLAGGING**

IF THE AUTOMATED FLAGGER ASSISTANCE DEVICE (AFAD) STOPS WORKING, FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT REMOVE TEMPORARY PORTABLE RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.

WHEN THE DISTANCE BETWEEN FLAGGERS EXCEEDS 2 MILES, A PILOT CAR IS REQUIRED. WHEN CURVES REDUCE SIGHT DISTANCE BELOW 400', A PILOT CAR IS REQUIRED.

- ① SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.
- ② IF FLAGGERS ARE PHYSICALLY NEEDED TO FLAG, REPLACE WO3-4 SIGNS WITH W20-7A SIGNS.

**TEMPORARY PORTABLE RUMBLE STRIPS**

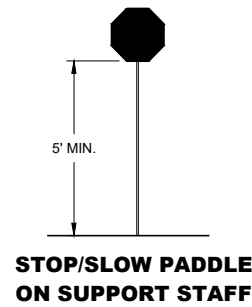
UTILIZE TEMPORARY PORTABLE RUMBLE STRIPS ON ALL FLAGGING OPERATIONS.

ONLY USE TEMPORARY PORTABLE RUMBLE STRIPS FROM THE APPROVED PRODUCTS LIST.

PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.

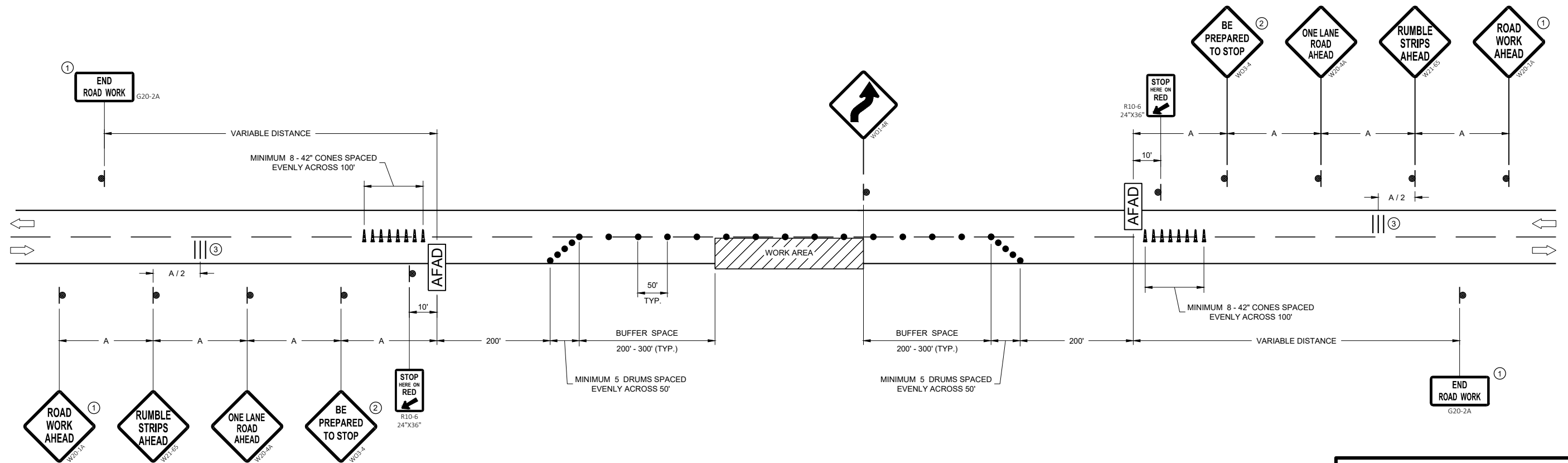
DO NOT INSTALL TEMPORARY PORTABLE RUMBLE STRIPS ON GRAVEL, MILLED SURFACES, OR ASPHALT THAT HAS BEEN PAVED LESS THAN 12 HOURS.

③ EACH TEMPORARY PORTABLE RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS PLACED TRANSVERSELY AT THE LOCATIONS SHOWN. WITHIN EACH ARRAY, SPACING BETWEEN RUMBLE STRIPS SHALL BE 15 FEET ON CENTER.



**SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE**

SPEED LIMIT	SPACING "A"
25-30 MPH	200'
35-40 MPH	350'
45-55 MPH	500'







**TRAFFIC CONTROL, LANE CLOSURE WITH AUTOMATED FLAGGER ASSISTANCE DEVICE**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
May 2022 /S/ Andrew Heidtke  
DATE WORK ZONE ENGINEER

FHWA

**LEGEND**

- V1 LEAD VEHICLE
- V2 MARKING VEHICLE
- V3 SHADOW VEHICLE
-  TRUCK MOUNTED ATTENUATOR (TMA)
-  SIGN ON TEMPORARY SUPPORT
-  DIRECTION OF TRAFFIC
-  FLASHING ARROW PANEL (CAUTION)

**GENERAL NOTES**

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

ALL VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL OPERATING IN CAUTION MODE. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.

DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL AND HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.

THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS.

WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR LAY STATIONARY SIGNS AND SUPPORTS FLAT ON THE GRADE WITH UPRIGHTS ORIENTED PARALLEL TO AND DOWNSTREAM FROM TRAFFIC.

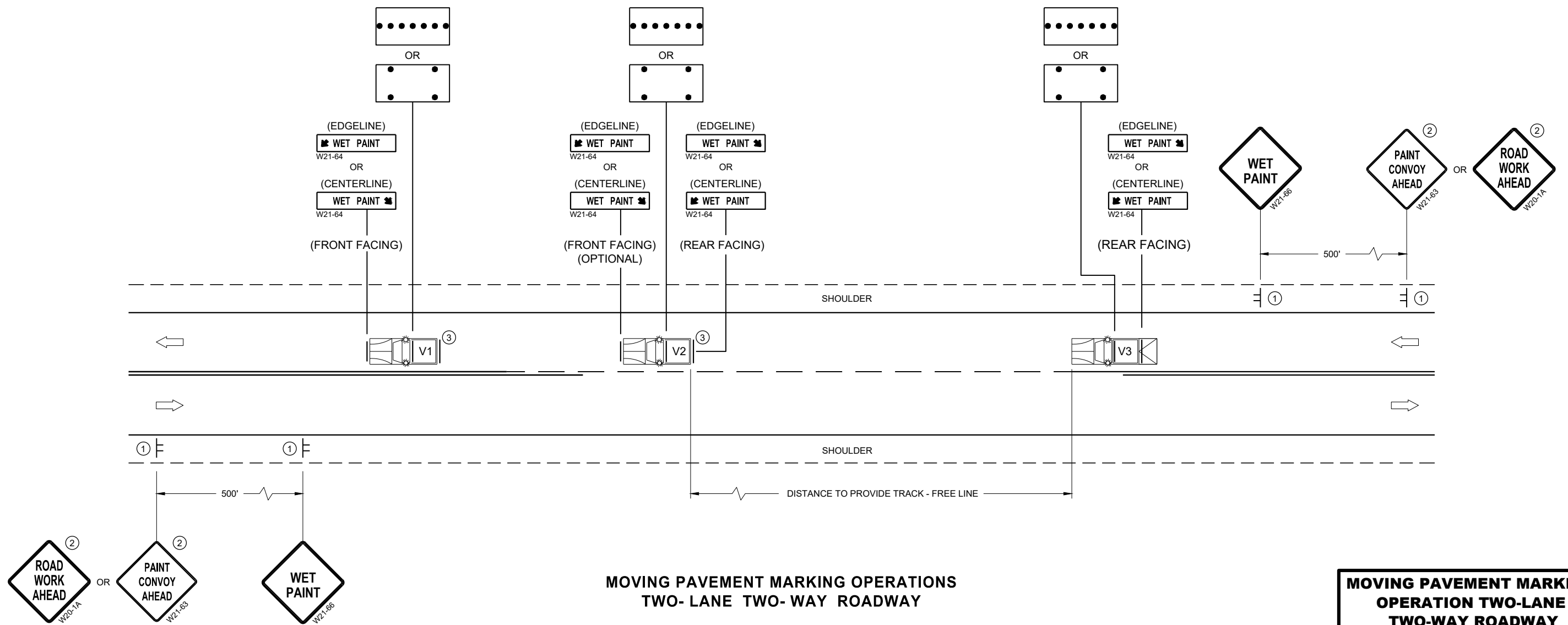
CONES SHOULD BE USED BETWEEN THE MARKING AND SHADOW VEHICLE AT 100 FOOT SPACING. CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.

CONES SHALL BE A MINIMUM OF 28" FOR WET PAVEMENT MARKING.

- ① SIGNS SHALL BE REPEATED APPROXIMATELY EVERY THREE MILES.
- ② IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1A OR W21-63 ARE NOT REQUIRED.
- ③ V1 AND V2 CAN BE SWITCHED SO THAT THE MARKER IS THE LEAD VEHICLE.

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**MOVING PAVEMENT MARKING OPERATIONS  
TWO-LANE TWO-WAY ROADWAY**

SDD 15C19 - 07a

SDD 15C19 - 07a

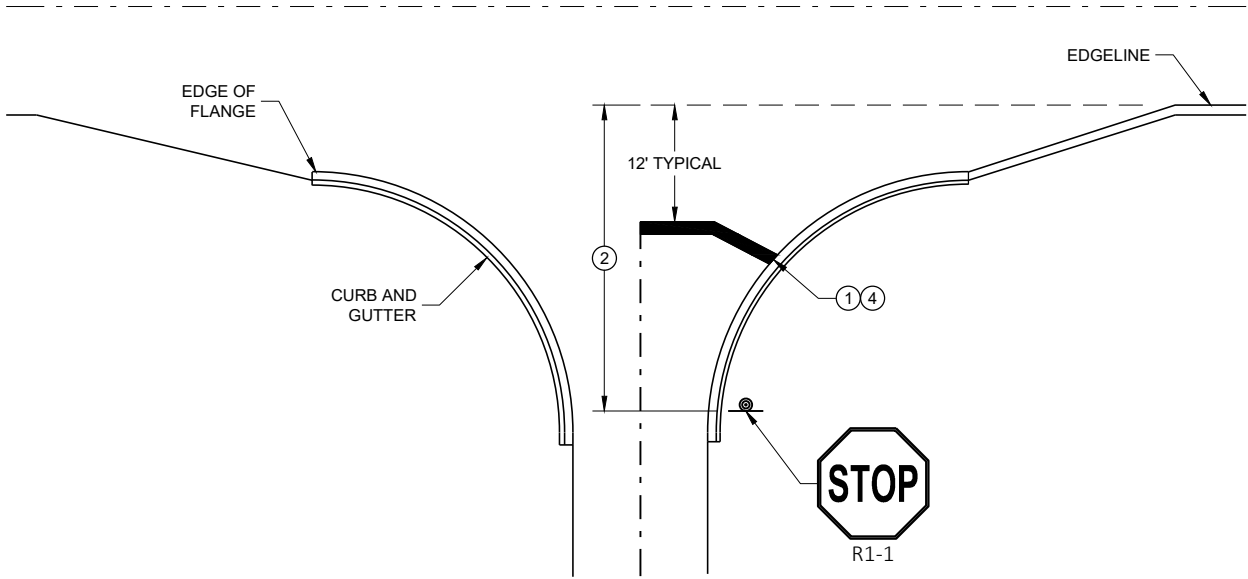
<b>MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY</b>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2022 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	



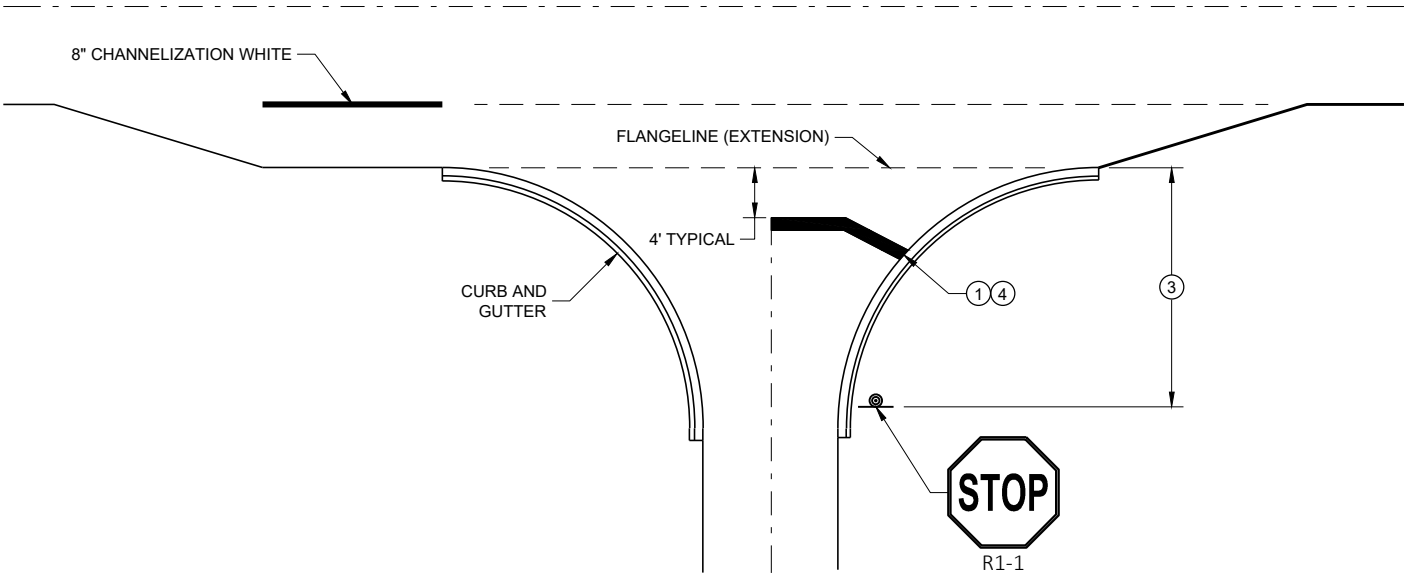
**GENERAL NOTES**

STOP SIGN SHALL BE PLACED A MINIMUM OF 6 FEET TO A MAXIMUM OF 50 FEET FROM THE EDGELINE LOCATION.

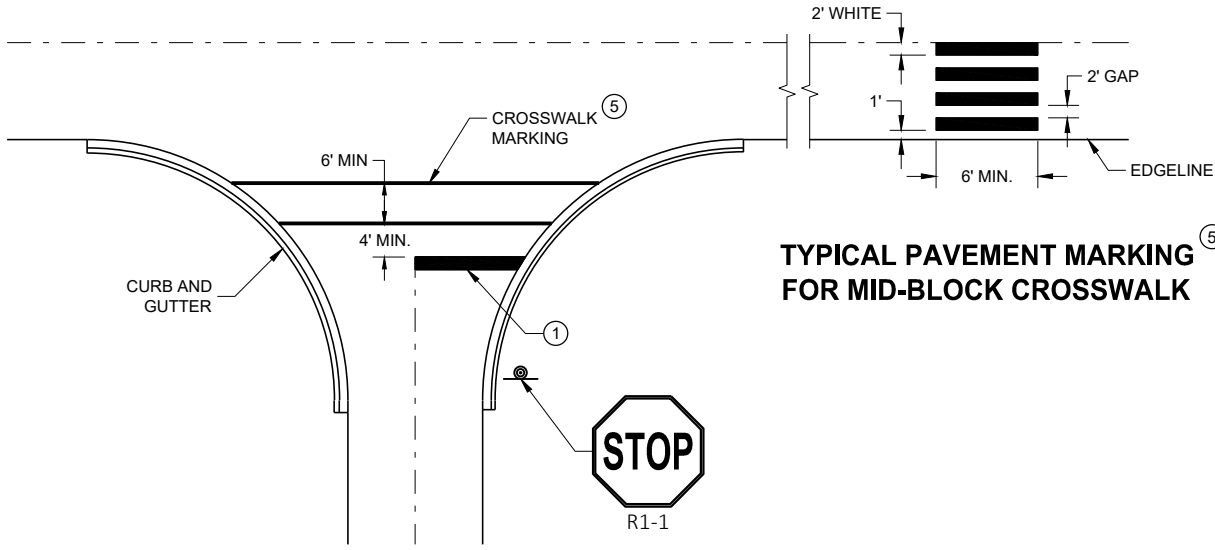
- ① 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE REGION MARKING ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
- ② NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGELINE.
- ③ NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGELINE EXTENSION.
- ④ MOVE CLOSER TO THE EDGE OF TRAVEL LINE AS NEEDED FOR VISIBILITY AND SIGHT LINES (NO CLOSER THAN 4 FEET).
- ⑤ LADDER BAR CROSSWALKS SHOULD ONLY BE USED FOR MID BLOCK CROSSINGS. USE 2 - 6" TRANSVERSE LINES INSTEAD.



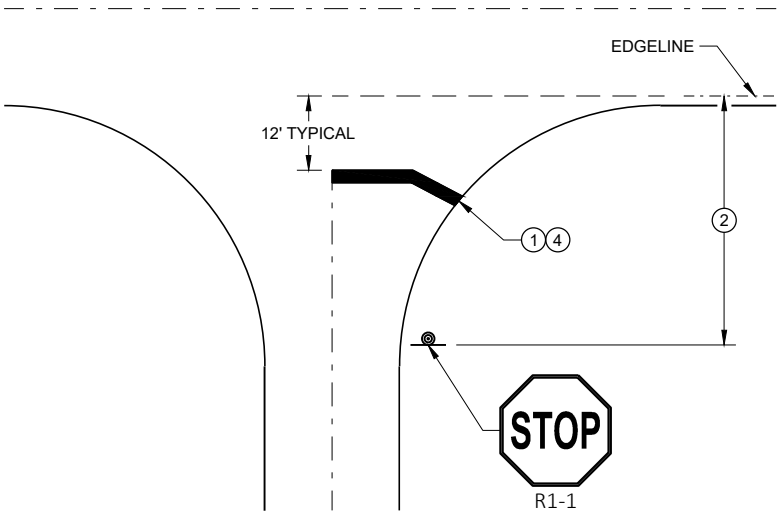
**TYPICAL STOP LINE PAVEMENT MARKING WITH CURB AND GUTTER**



**TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH RIGHT TURN LANE**



**TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH CROSSWALK MARKING**



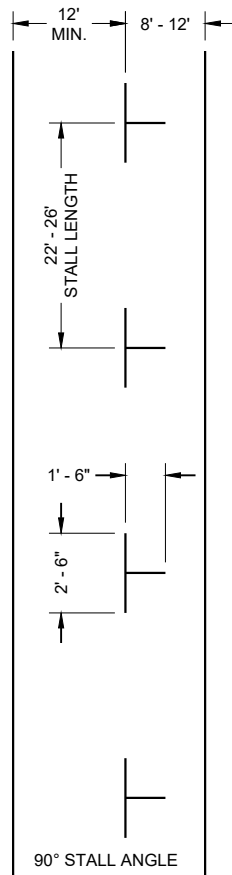
**TYPICAL STOP LINE PAVEMENT MARKING WITHOUT CURB AND GUTTER**

**STOP LINE AND CROSSWALK PAVEMENT MARKING**

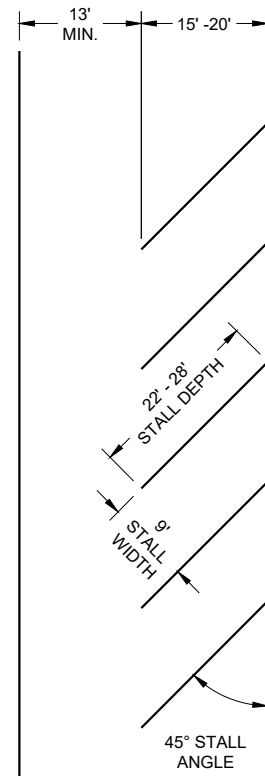
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
November 2019 /S/ Matthew Rauch  
DATE STATE SIGNING AND MARKING ENGINEER

FHWA

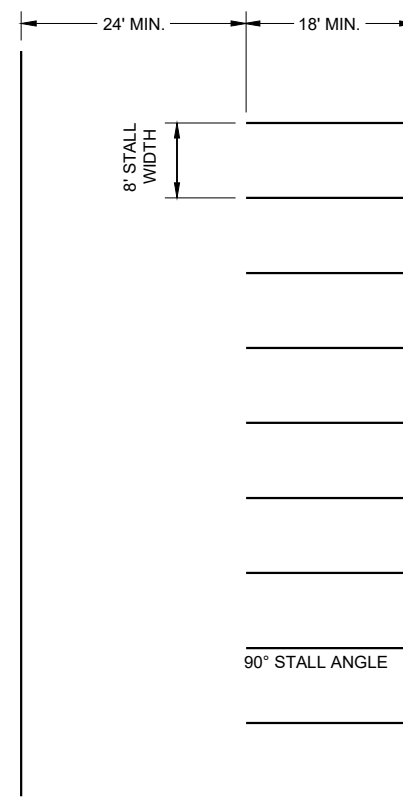
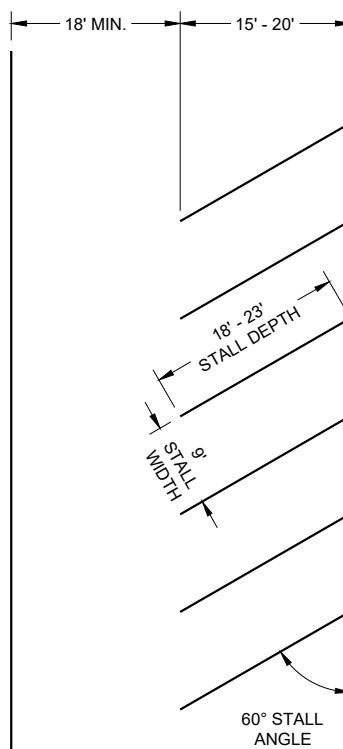


**PARALLEL PARKING**



**ANGLED PARKING**

(ANGLED PARKING IS NOT ALLOWED ON STATE HIGHWAYS UNLESS A DESIGN JUSTIFICATION HAS BEEN COMPLETED.)



**PARKING LOTS**

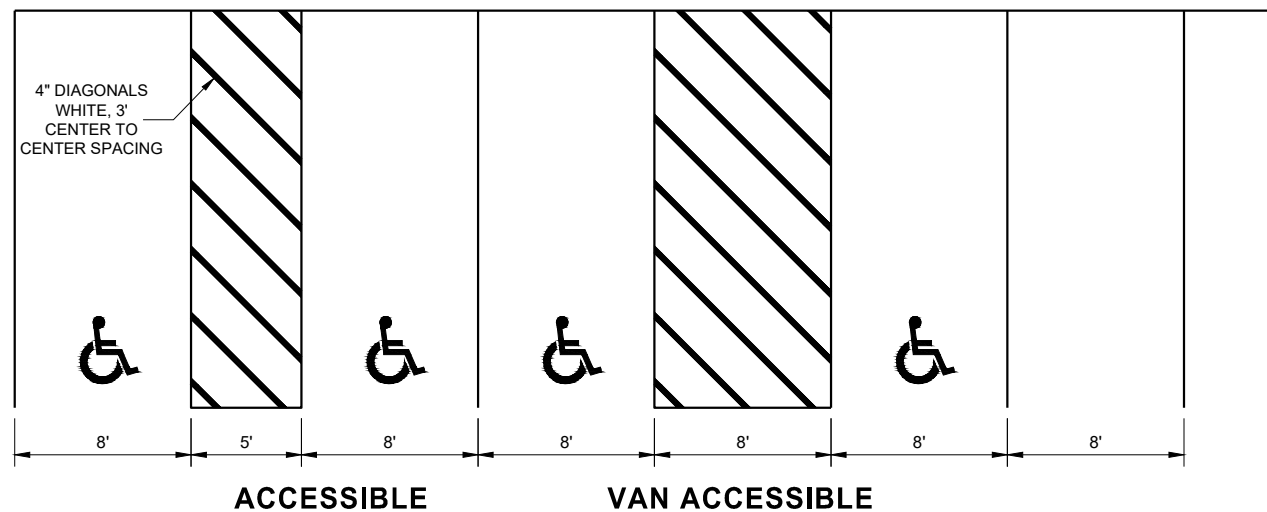
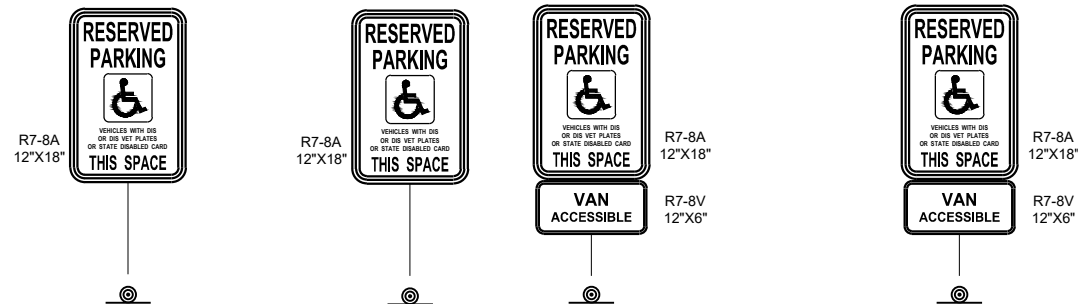
**GENERAL NOTES**

ALL LINES 4" WHITE (UNLESS OTHERWISE NOTED)

LAST PARKING STALL IS A MINIMUM OF 15' FROM THE CROSSWALK.





**LEGEND**

⊙ SIGN ON PERMANENT SUPPORT



PARKING STALL MARKING	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED August 2019 DATE	/s/ Matthew Rauch STATE SIGNING AND MARKING ENGINEER
FHWA	

**LEGEND**

-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  DIRECTION OF TRAFFIC
-  WORK ZONE

**GENERAL NOTES**

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" SIGNS MAY BE USED IF APPROVED BY THE REGIONAL TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

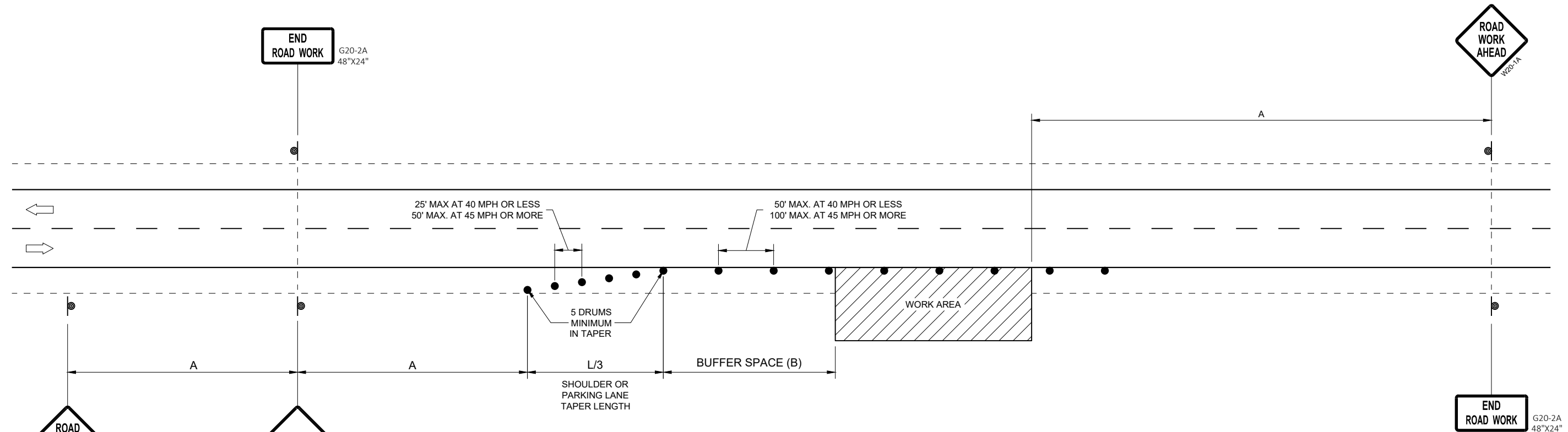
CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

W20-1A AND G20-2A SIGNS ARE NOT REQUIRED IF THE WORK AREA IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT. G20-2A SIGNS MAY ALSO BE OMITTED IF DURATION OF WORK IS LESS THAN 7 CONTINUOUS DAYS AND NIGHTS.

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POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET	SHOULDER TAPER L / 3 W, LATERAL OFFSET (FT)						BUFFER SPACE (B) FEET
		3	4	5	6	7	8	
25	200'	10	14	17	21	24	28	55
30	200'	15	20	25	30	35	40	85
35	350'	20	27	34	40	47	54	120
40	350'	26	35	44	53	62	70	170
45	500'	45	59	74	89	104	119	220
50	500'	50	66	83	99	116	132	280
55	500'	54	73	91	109	127	145	335'

SDD 15D28 - 04

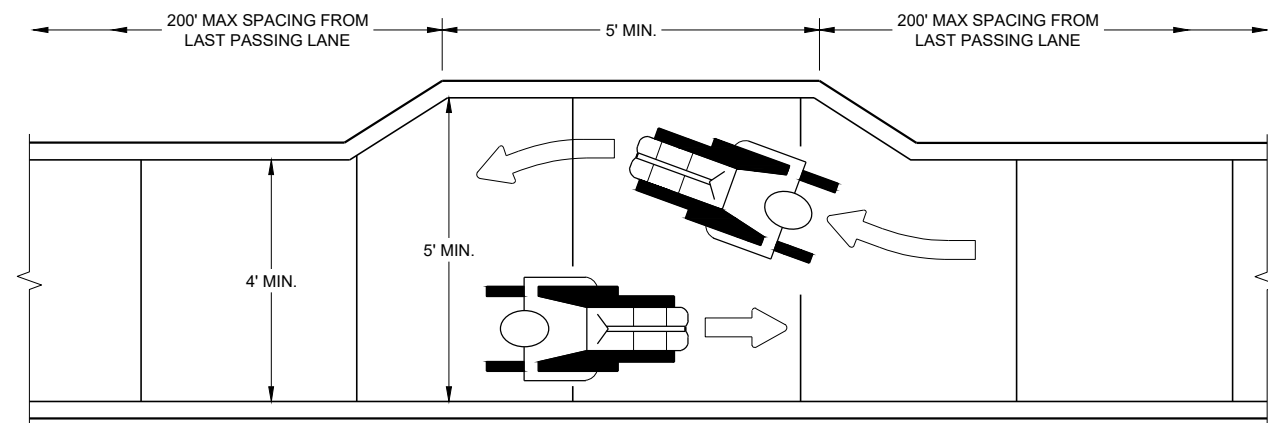
SDD 15D28 - 04

**TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY**

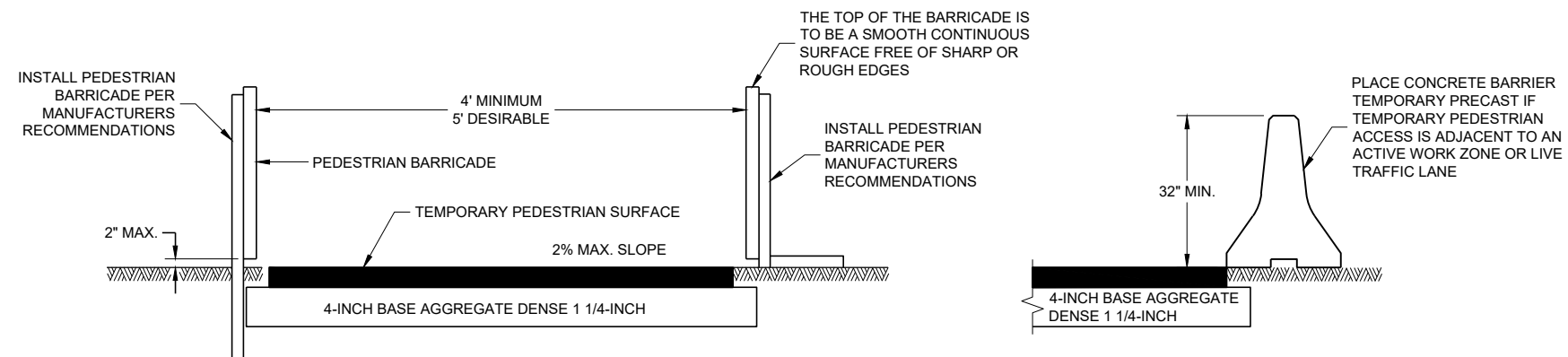
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
May 2020 /S/ Andrew Heidtke  
DATE STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER

FHWA



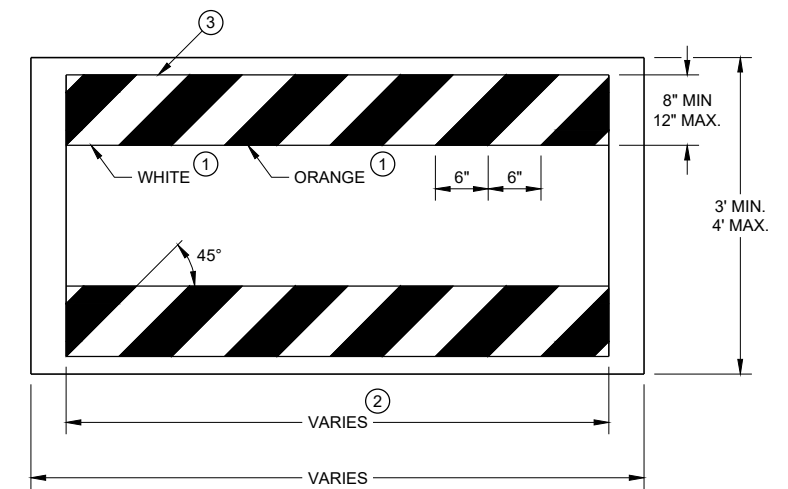
**NARROW SIDEWALK PASSING DETAIL**



**TEMPORARY PEDESTRIAN ACCESS**

**GENERAL NOTES**

- BARRICADE DEVICE SELECTED FROM APPROVED PRODUCT LIST
- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② SHEETING REQUIRED ON MORE THAN 50% OF BARRICADE WIDTH.
- ③ PLACE SHEETING ON BOTH SIDES OF THE BARRICADE.
- \* USE THIS DETAIL FOR SHEETING PLACEMENT REFERENCE.

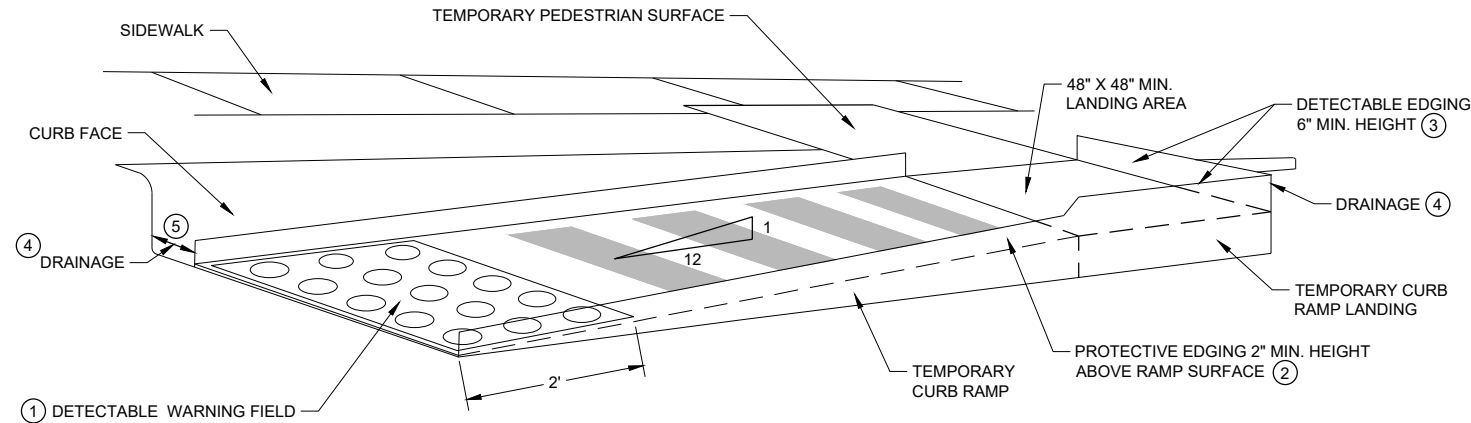


**TEMPORARY PEDESTRIAN BARRICADE\***

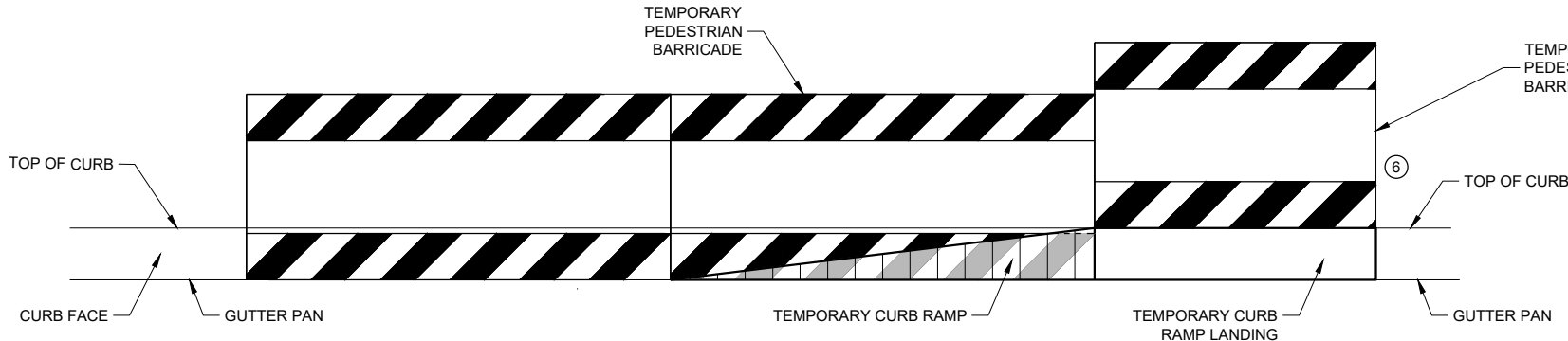
**GENERAL NOTES**

CURB RAMPS SHALL BE 48" MIN. WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE.  
 CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.  
 CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.  
 LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.  
 CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES MAY BE VERTICAL UP TO 1/4" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".

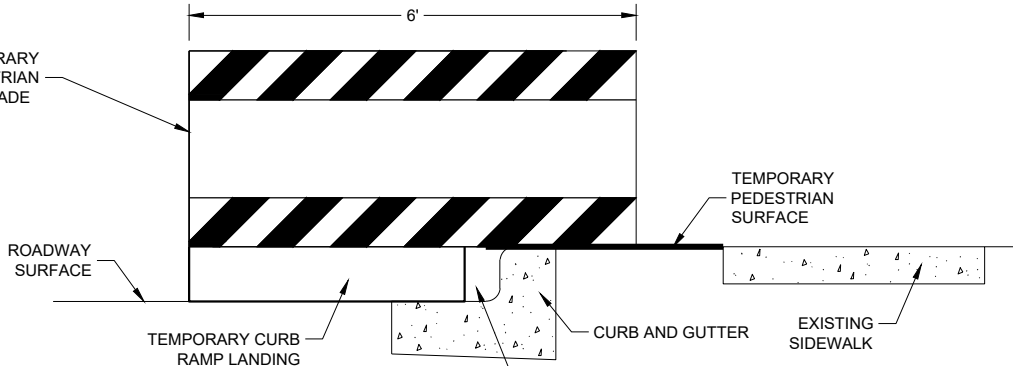
- ① INSTALL CONTRASTING TEMPORARY DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS, AS SHOWN IN THE PLANS.
- ② PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- ③ DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- ④ DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ⑤ 6" MINIMUM BETWEEN CURB FACE AND EDGE OF RAMP
- ⑥ IF ONLY PART OF THE END PANEL OF TEMPORARY PEDESTRIAN BARRICADE PANEL IS NEEDED, EXTEND EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL HERE.



**PERSPECTIVE VIEW**



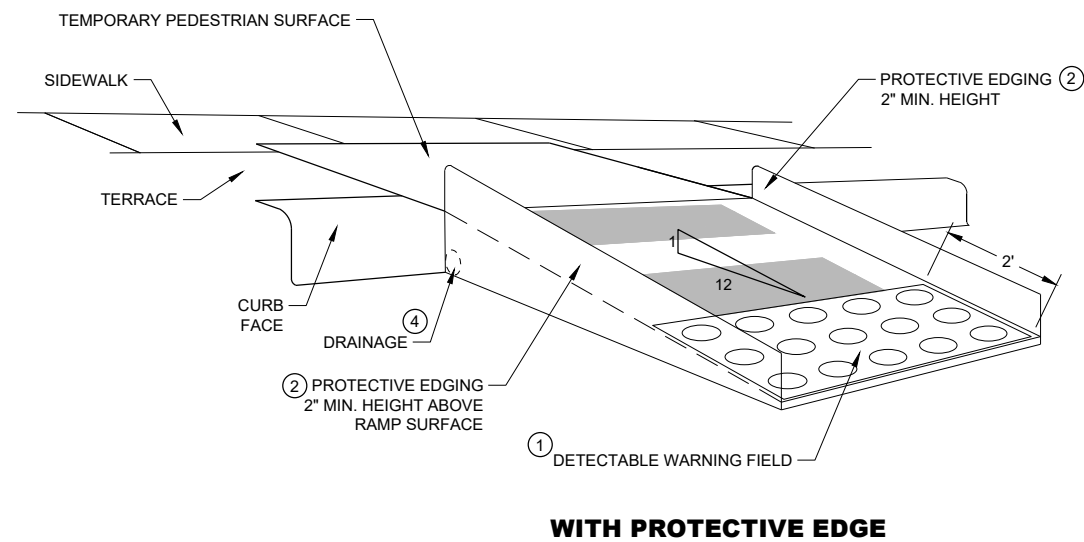
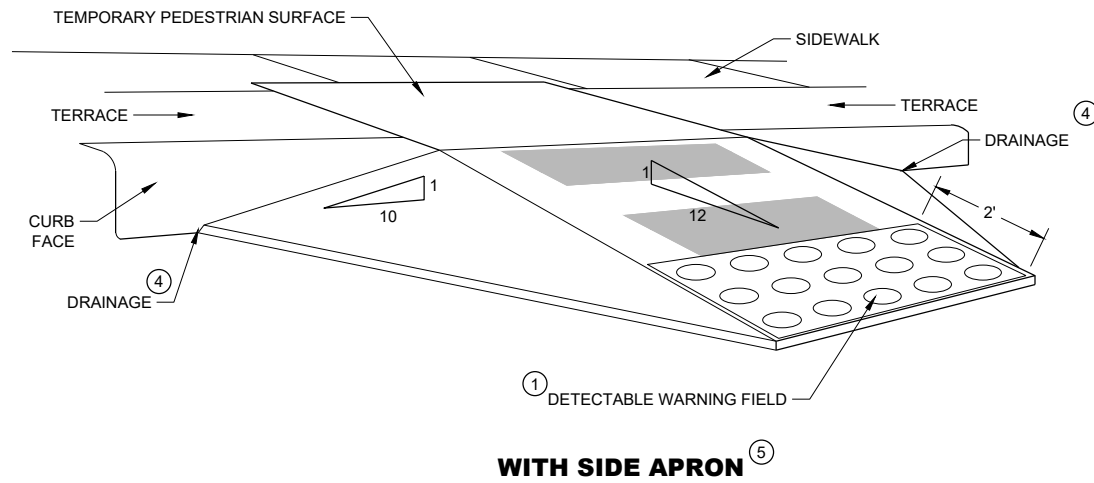
**FRONT VIEW**



**SIDE VIEW**

**TEMPORARY CURB RAMP PARALLEL TO CURB**

<p><b>TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION</b></p>
<p>STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION</p>



**TEMPORARY CURB RAMP PERPENDICULAR TO CURB**

**GENERAL NOTES**

CURB RAMPS SHALL BE 48" MINIMUM WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE.

ALTERNATE SIDEWALK WORK BETWEEN LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.

CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.

CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.

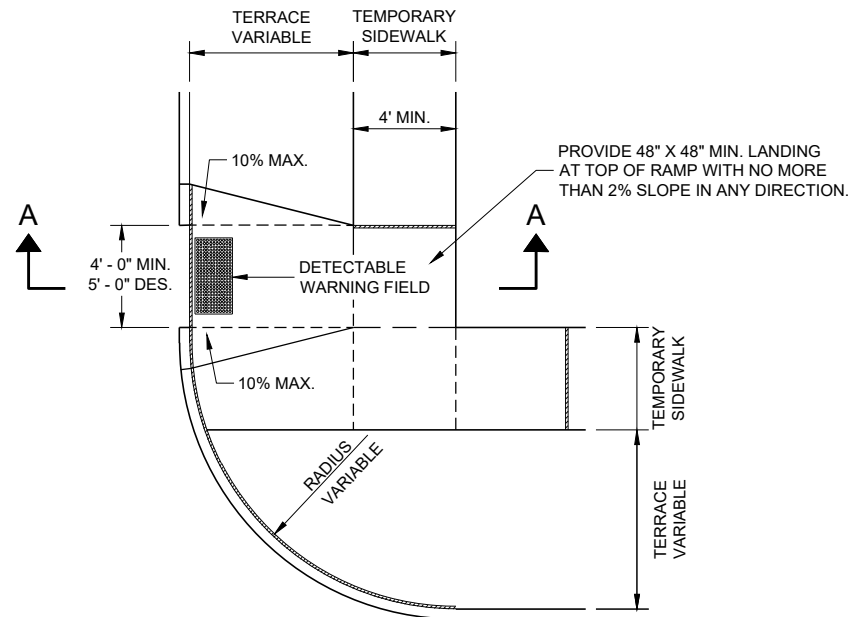
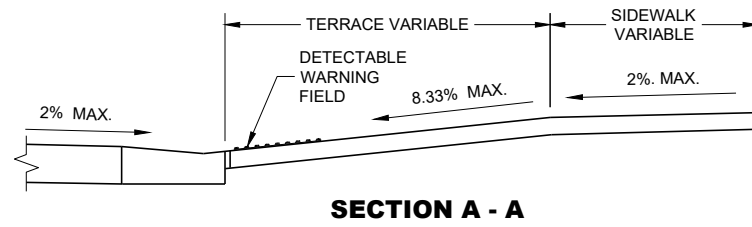
LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.

CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES MAY BE VERTICAL UP TO 1/4" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".

- ① INSTALL CONTRASTING TEMPORARY DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS, AS SHOWN IN THE PLANS
- ② PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- ③ DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- ④ DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ⑤ CAN ONLY BE USED FOR RAMPS WITH 6" OR LESS OF VERTICAL CHANGE.

**GENERAL NOTES**

- BARRICADE DEVICE SELECTED FROM APPROVED PRODUCT LIST
- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② SHEETING REQUIRED ON MORE THAN 50% OF BARRICADE WIDTH.
- ③ PLACE SHEETING ON BOTH SIDES OF THE BARRICADE.
- ★ USE THIS DETAIL FOR SHEETING PLACEMENT REFERENCE.



**PLAN VIEW**  
**TEMPORARY TYPE 3 RAMP**  
 (OUTSIDE OF CROSSWALK AREA)

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SDD 15D30 - 07d

SDD 15D30 - 07d

<b>TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION</b>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2022 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

**GENERAL NOTES**

TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.

NOTIFY THE BUS COMPANY 7 DAYS IN ADVANCE OF THE BUS STOP RELOCATION.

PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMP OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.

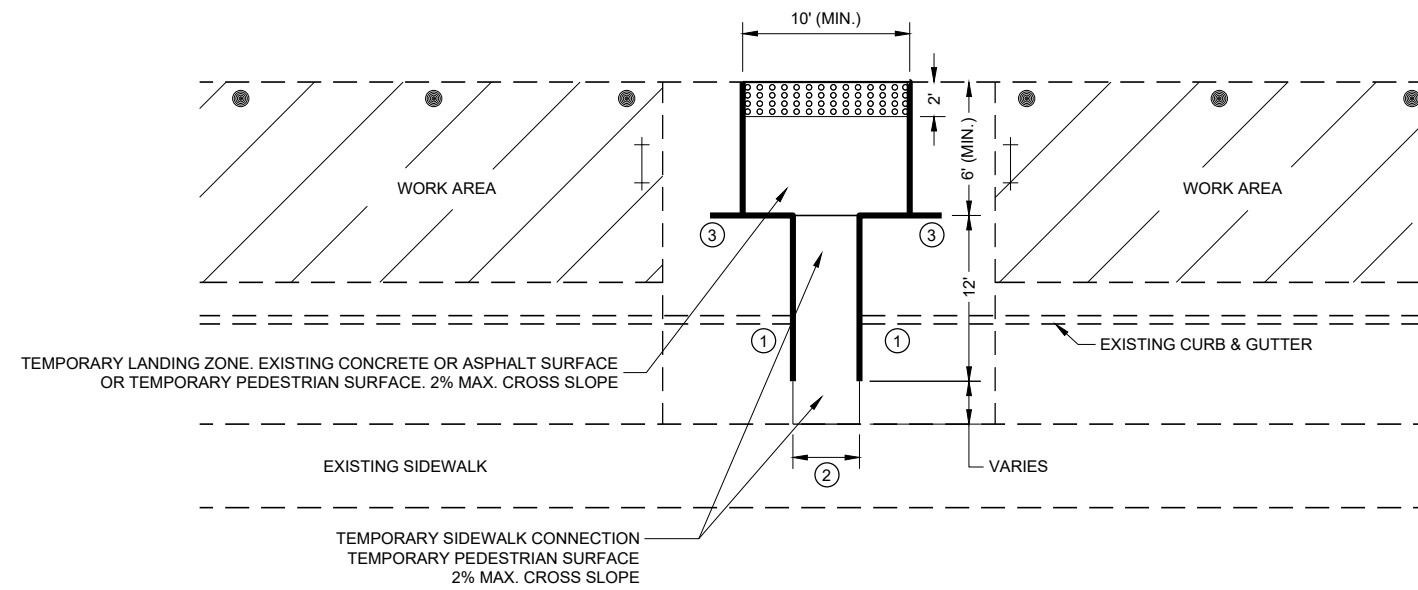
DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).

LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.

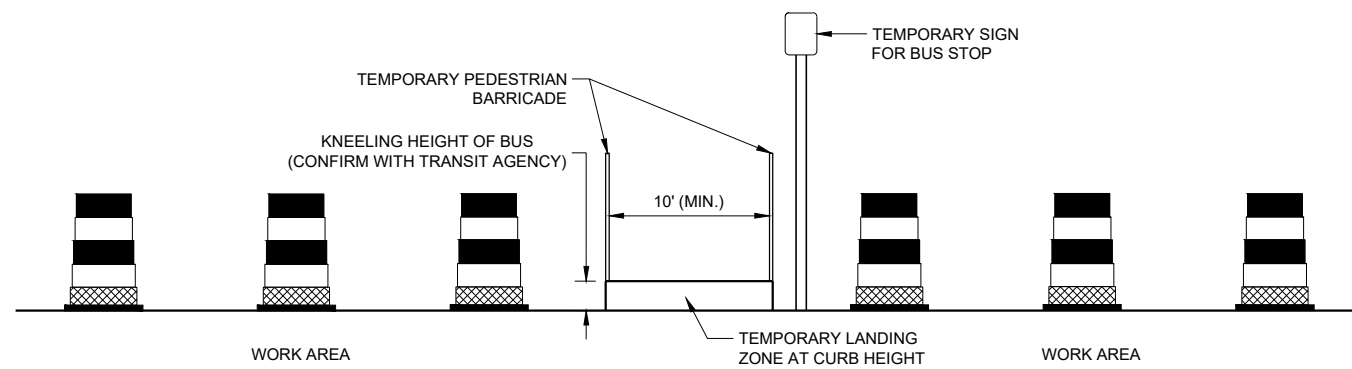
CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES MAY BE VERTICAL UP TO 1/4" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".

CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.

- ① DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ② 5' WIDE MIN. WITH TEMPORARY PEDESTRIAN BARRICADE, 10' WIDE MIN. WITHOUT TEMPORARY PEDESTRIAN BARRICADE.
- ③ PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE INTO THIS SPACE.



**PLAN VIEW**



**PROFILE VIEW  
TEMPORARY BUS STOP PAD**

**LEGEND**







- TRAFFIC CONTROL DRUM
- ⊥ TYPE III BARRICADE
- ▬ TEMPORARY PEDESTRIAN BARRICADE
- ⊞ TEMPORARY DETECTABLE WARNING FIELD
- ▨ WORK AREA

**TRAFFIC CONTROL,  
PEDESTRIAN ACCOMMODATION**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

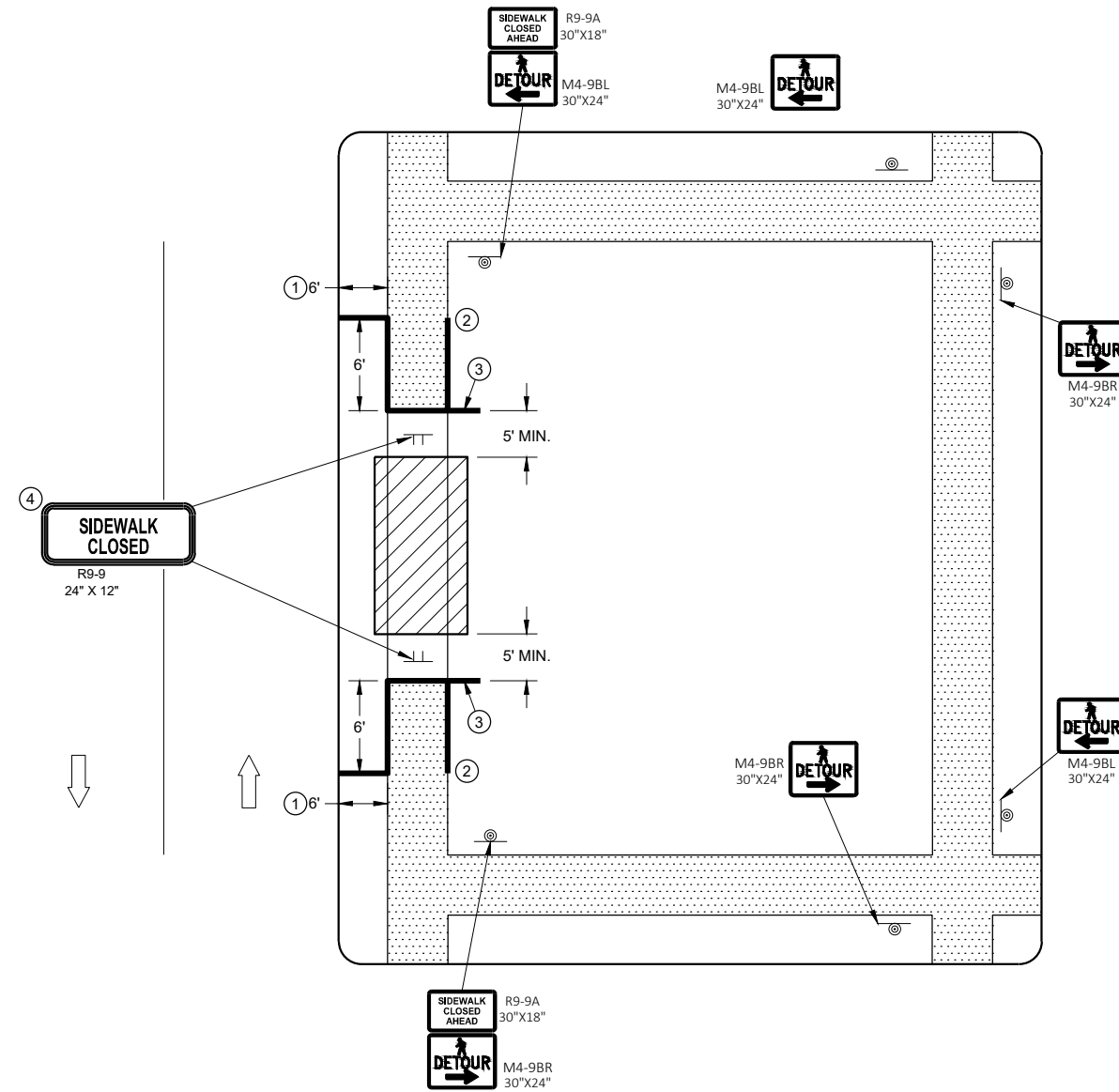


**LEGEND**

-  SIGN ON PERMANENT SUPPORT
-  SIGN ON TEMPORARY SUPPORT
-  UNDER PEDESTRIAN TRAFFIC
-  WORK AREA
-  TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC

**GENERAL NOTES**








- SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.
- WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.
- SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.
- ① IF TERRACE IS LESS THAN 6 FEET WIDE, OMIT TEMPORARY PEDESTRIAN BARRICADE FROM THE SIDEWALK TO THE CURB.
  - ② PLACE BARRICADE CLOSURE SO THAT THE TEMPORARY PEDESTRIAN BARRICADE END IS AT THE LAST OPEN SIDEWALK ACCESS TO RESIDENCES OR BUSINESSES BEFORE THE SIDEWALK CLOSURE.
  - ③ IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
  - ④ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.



**SIDEWALK DETOUR, SIDEWALK ONLY ON ONE SIDE**

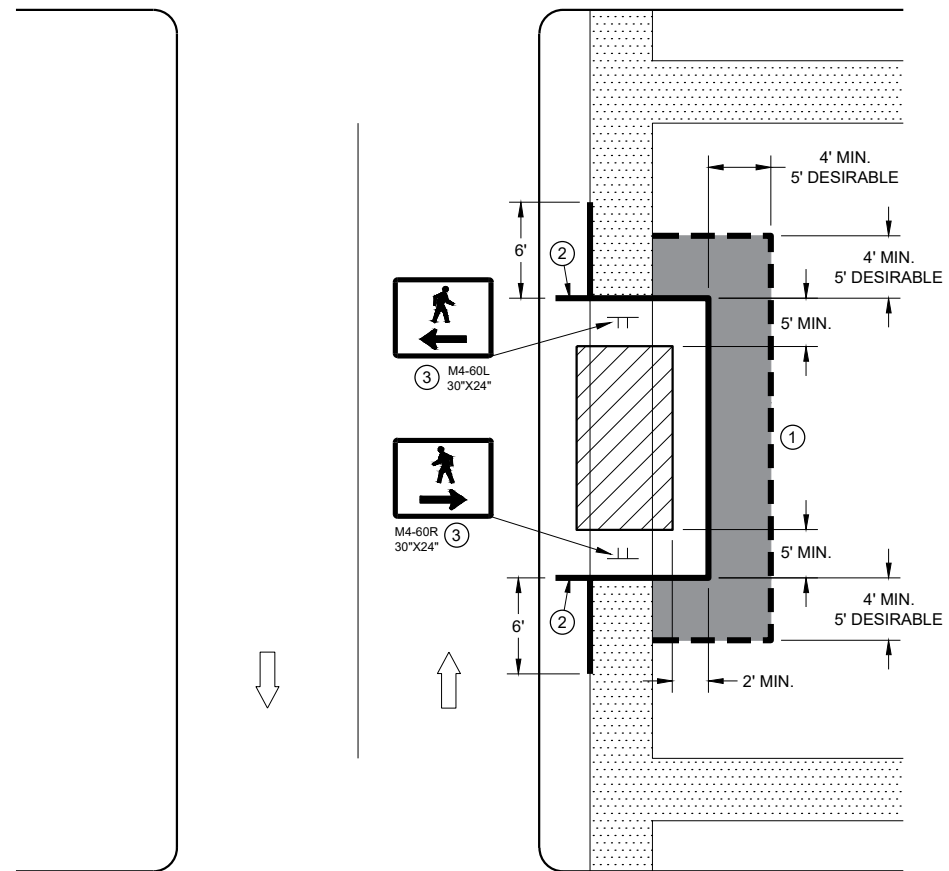
<b>TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION</b>
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

**LEGEND**

-  SIGN ON TEMPORARY SUPPORT
-  WORK AREA
-  UNDER PEDESTRIAN TRAFFIC
-  TEMPORARY PEDESTRIAN SURFACE
-  TEMPORARY PEDESTRIAN BARRICADE
-  OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC










**GENERAL NOTES**

- TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.
- SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.
- WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.
- SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.
- ① USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
- ② IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
- ③ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.



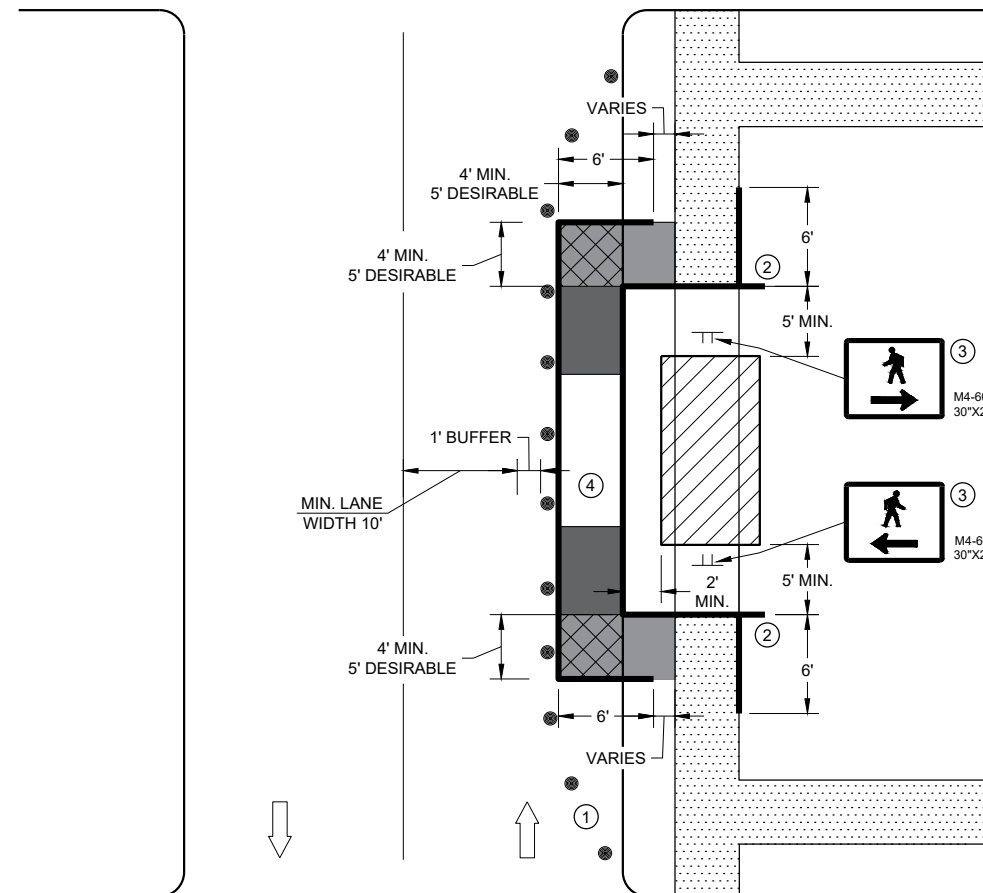
**SIDEWALK DIVERSION  
SINGLE SIDE**

**LEGEND**

-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL DRUM
-  WORK AREA
-  UNDER PEDESTRIAN TRAFFIC
-  TEMPORARY CURB RAMP
-  TEMPORARY PEDESTRIAN SURFACE "A"
-  TEMPORARY PEDESTRIAN SURFACE "B"
-  TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC

**GENERAL NOTES**

- TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.
- SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.
- WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.
- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND BUFFER SPACE REQUIRED.
  - ② PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL PAST THE SIDEWALK ON THE SIDE AWAY FROM THE ROAD.
  - ③ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.
  - ④ USE EXISTING PAVEMENT SURFACE. IF EXISTING PAVEMENT SURFACE HAS BEEN REMOVED, USE A TEMPORARY PEDESTRIAN SURFACE.



**SIDEWALK DIVERSION, SINGLE SIDE**

<b>TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION</b>
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

6

6

SDD 15D30 - 07h

SDD 15D30 - 07h

### GENERAL NOTES

IF PEDESTRIAN PUSH BUTTONS ARE PRESENT ON THE EXISTING FACILITY, ENSURE THEY ARE MAINTAINED/ACCESSIBLE FOR PEDESTRIAN USE THROUGHOUT THE TEMPORARY PEDESTRIAN ACCOMMODATIONS.











SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

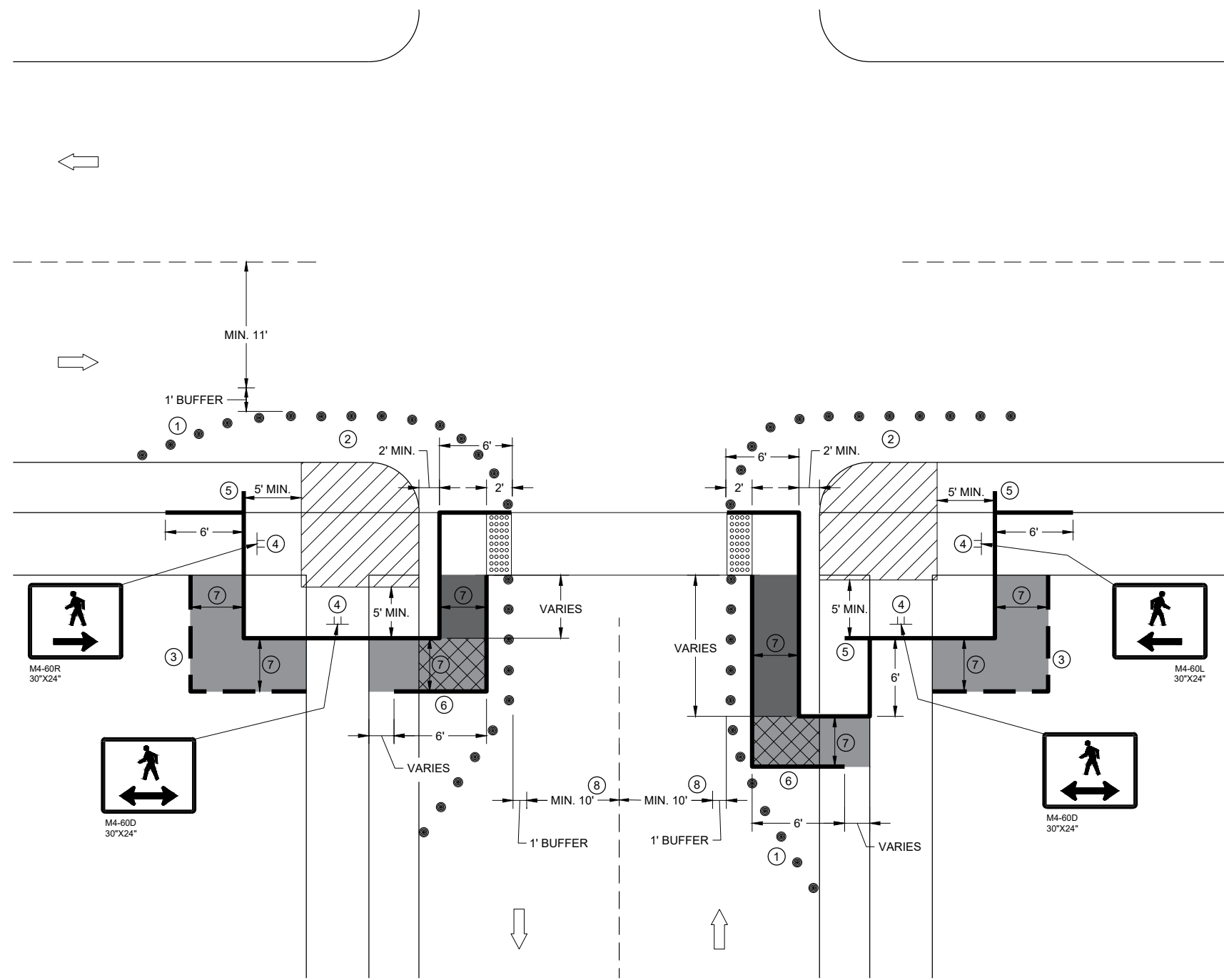
TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG

WHEN TEMPORARY PEDESTRIAN BARRICADE RUNS PARALLEL ALONG THE SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND PROPER BUFFER SPACE REQUIRED.
- ② PROVIDE ADEQUATE SPACE FOR CONTRACTOR OPERATIONS
- ③ USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
- ④ MOUNTING HEIGHT OF 5 FEET FROM SIDEWALK SURFACE TO BOTTOM OF SIGN.
- ⑤ PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL IN THE SIDEWALK TERRACE.
- ⑥ IF TEMPORARY PEDESTRIAN BARRICADE DOES NOT REACH THE FACE OF THE CURB, USE AN ADDITIONAL PANEL AND EXTEND INTO THE TERRACE.
- ⑦ 4 FEET MINIMUM, 5 FEET DESIRABLE
- ⑧ IF MINIMUM LANE WIDTHS CAN'T BE ATTAINED, CURB RAMPS MAY NEED TO BE CONSTRUCTED AT SEPARATE TIMES.

### LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL DRUM
-  WORK AREA
-  TEMPORARY CURB RAMP
-  TEMPORARY PEDESTRIAN SURFACE "A"
-  TEMPORARY PEDESTRIAN SURFACE "B"
-  TEMPORARY DETECTABLE WARNING FIELD
-  TEMPORARY PEDESTRIAN BARRICADE
-  OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC



**CURB RAMP PEDESTRIAN TRAFFIC CONTROL  
SIDEWALK ON SINGLE SIDE**

<b>TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION</b>
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

6

6

SDD 15D30 - 07i

SDD 15D30 - 07i

**GENERAL NOTES**

IF PEDESTRIAN PUSH BUTTONS ARE PRESENT ON THE EXISTING FACILITY, ENSURE THEY ARE MAINTAINED/ACCESSIBLE FOR PEDESTRIAN USE THROUGHOUT THE TEMPORARY PEDESTRIAN ACCOMMODATIONS.











SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

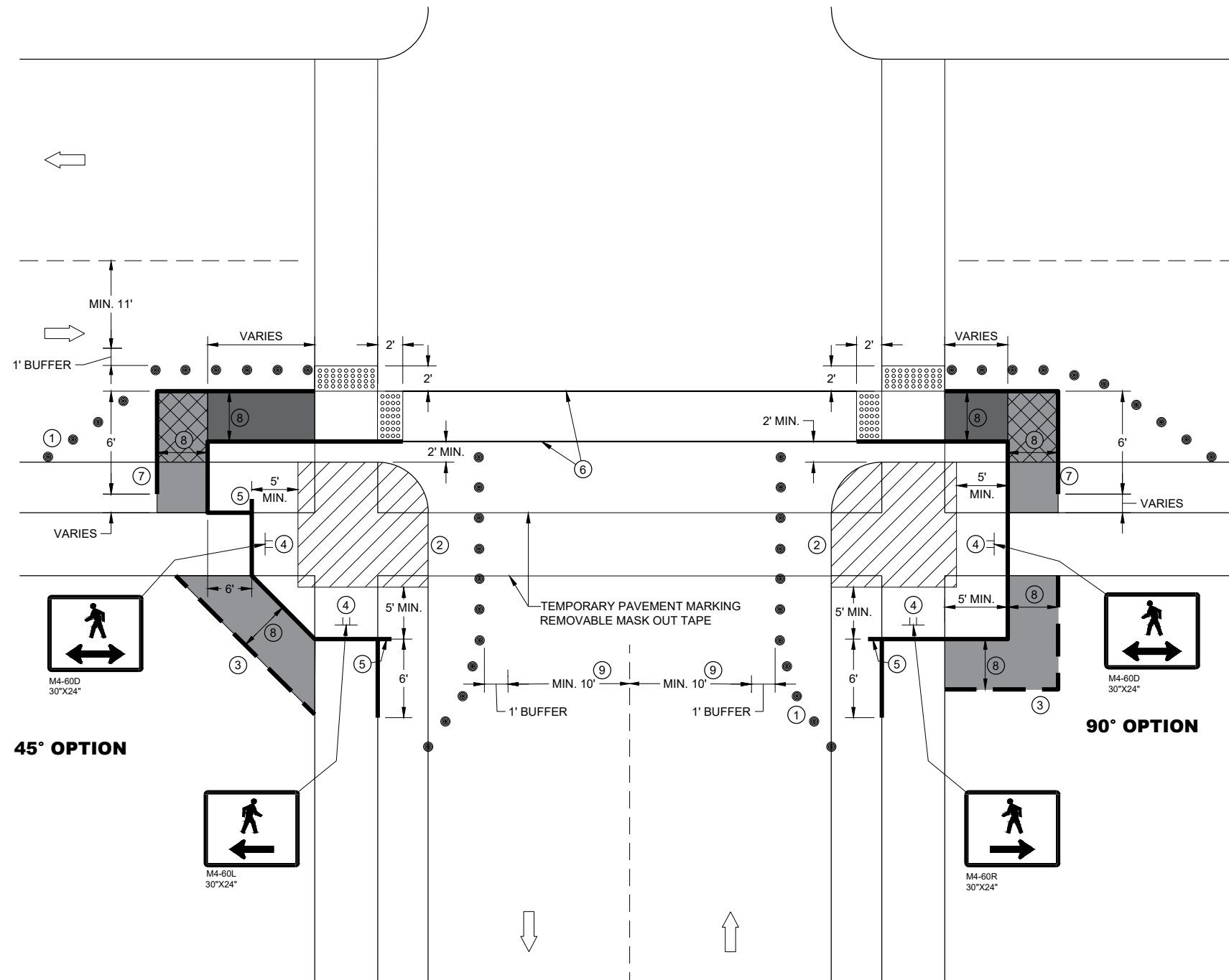
TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG

WHEN TEMPORARY PEDESTRIAN BARRICADE RUNS PARALLEL ALONG THE SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND PROPER BUFFER SPACE REQUIRED.
- ② PROVIDE ADEQUATE SPACE FOR CONTRACTOR OPERATIONS
- ③ USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
- ④ MOUNTING HEIGHT OF 5 FEET FROM SIDEWALK SURFACE TO BOTTOM OF SIGN.
- ⑤ PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL IN THE SIDEWALK TERRACE.
- ⑥ WHITE 6" TEMPORARY PAVEMENT MARKING
- ⑦ IF TEMPORARY PEDESTRIAN BARRICADE DOES NOT REACH THE FACE OF THE CURB, USE AN ADDITIONAL PANEL AND EXTEND INTO THE TERRACE.
- ⑧ 4 FEET MINIMUM, 5 FEET DESIRABLE
- ⑨ IF MINIMUM LANE WIDTHS CAN'T BE ATTAINED, CURB RAMPS MAY NEED TO BE CONSTRUCTED AT SEPARATE TIMES.

**LEGEND**

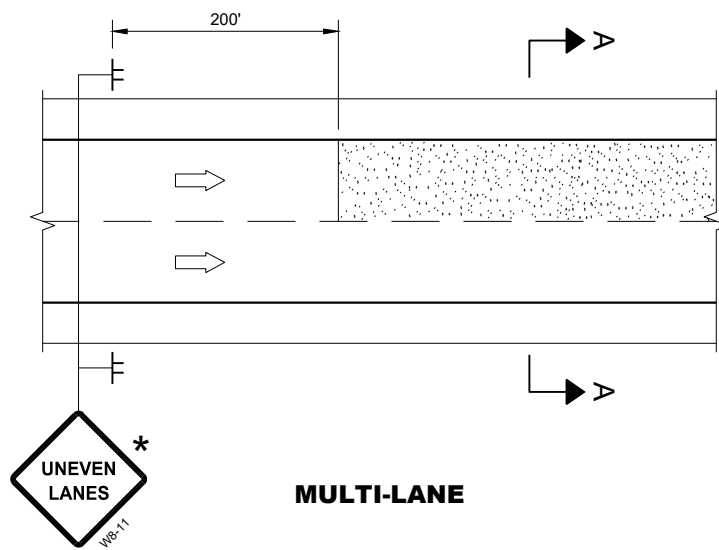
-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL DRUM
-  WORK AREA
-  TEMPORARY CURB RAMP
-  TEMPORARY PEDESTRIAN SURFACE "A"
-  TEMPORARY PEDESTRIAN SURFACE "B"
-  TEMPORARY DETECTABLE WARNING FIELD
-  TEMPORARY PEDESTRIAN BARRICADE
-  OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC



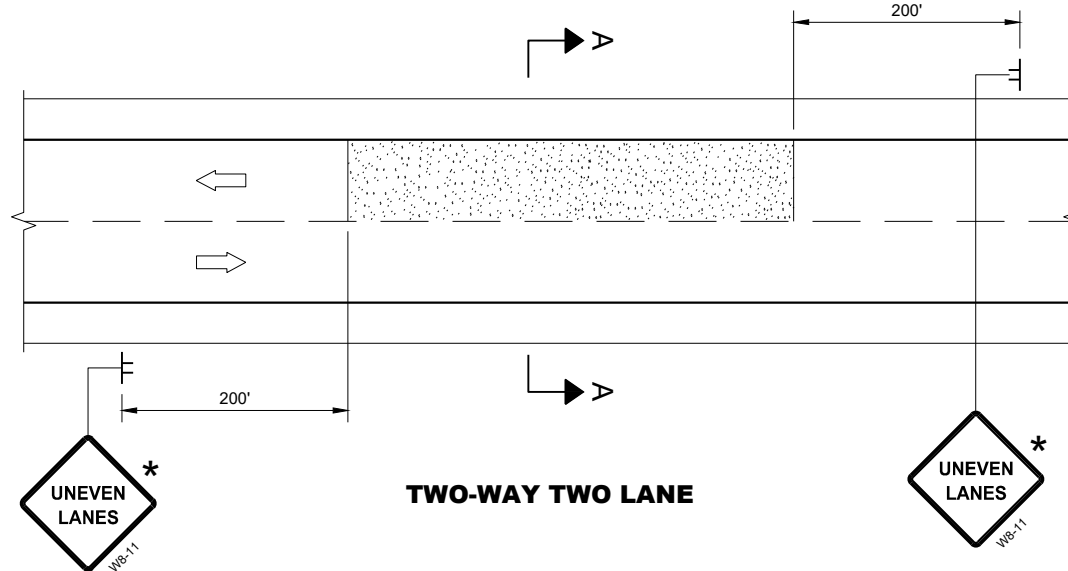
**CURB RAMP PEDESTRIAN TRAFFIC CONTROL**

**TRAFFIC CONTROL,  
PEDESTRIAN ACCOMMODATION**

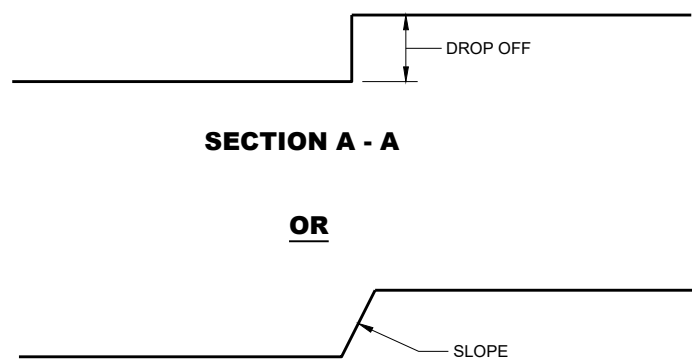
STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION



**MULTI-LANE**



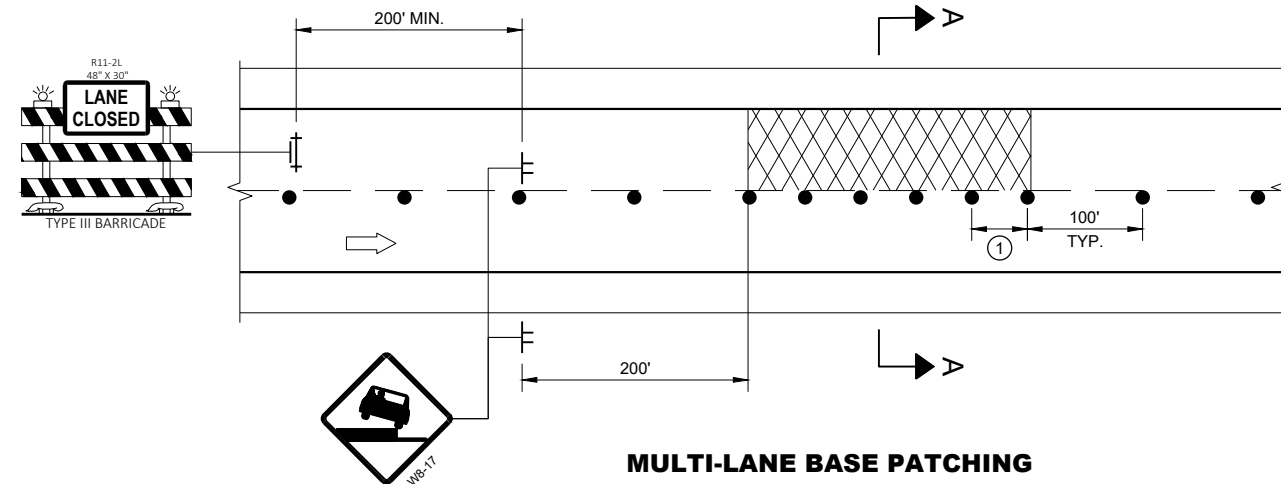
**TWO-WAY TWO LANE**



**SECTION A - A**

**OR**

**SECTION A - A**



**MULTI-LANE BASE PATCHING**

**ADJACENT LANE DROP-OFFS**

**GENERAL NOTES**

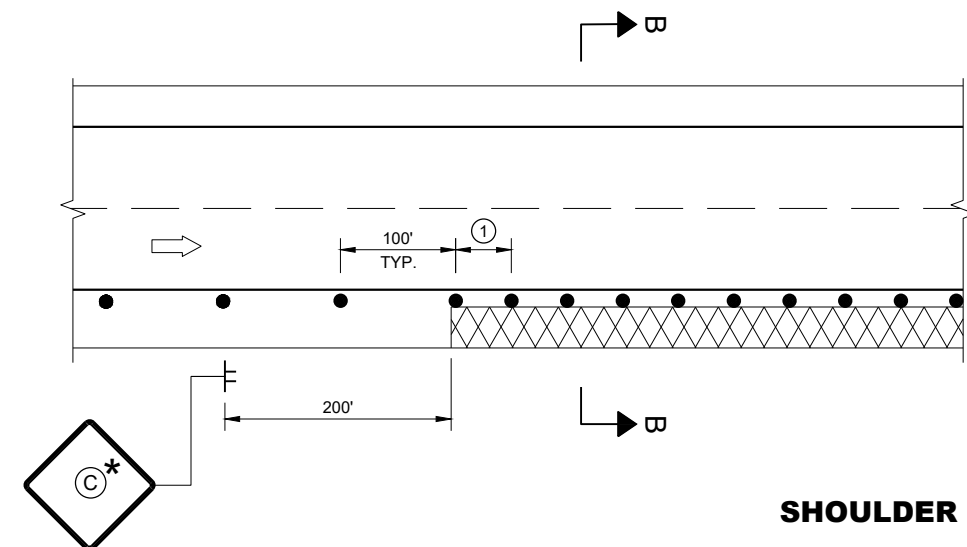
- FOR SPOT LOCATIONS USE ENGINEERING JUDGEMENT WHEN PLACING ADDITIONAL SIGNS.
- ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.
- "WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.
- WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.
- \* IF THE DROP-OFF IS CONTINUOUS ALONG THE PROJECT, PLACE ADDITIONAL SIGNS EVERY 1 MILE AND AFTER EVERY ENTRANCE RAMP.
- ① USE CLOSER SPACING WHEN DELINEATING DROP-OFF.

**LEGEND**

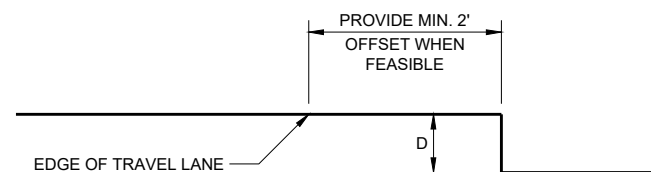
- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL DRUM
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- DIRECTION OF TRAFFIC
- WORK AREA WITH DROP-OFF
- MILLED SURFACE

6

6



**SHOULDER DROP-OFFS**



**SECTION B - B**

D	SIGN (C)
< 2" WITH A SLOPE STEEPER THAN 3:1	 LOW SHOULDER WO8-9
2" < 6" WITH A SLOPE STEEPER THAN 3:1	 SHOULDER DROP - OFF W8-9A PROVIDE A 3:1 OR FLATTER SLOPE OF MATERIAL ADJACENT TO THE PAVEMENT

SDD 15D39 - 02

SDD 15D39 - 02

**TRAFFIC CONTROL,  
DROP-OFF SIGNING**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
March 2018 /S/ Andrew Heidtke  
DATE DATE WORK ZONE ENGINEER

FHWA

**GENERAL NOTES**

DRAWING NOT TO SCALE. ALL SIGNS AND POSTS ON THIS SHEET SHALL BE PAID FOR WITH 'TRAFFIC CONTROL SIGNS' BID ITEM. ALL SIDE ROADS WHICH ARE UNDER CONSTRUCTION OF CURB AND GUTTER AND/OR GRADING SHALL BE ADEQUATELY SIGNED.

ALL SIGNS AND DEVICES SHALL BE IN CONFORMANCE WITH THE WISCONSIN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD). SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISDOT STANDARD SIGN PLATES.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THAT THE BACKGROUND IS ORANGE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

ALL SIGNS INAPPROPRIATE TO THE STATUS OF THE CONTROL ZONE, INCLUDING PRE-EXISTING SIGNS IN THE VICINITY, SHALL BE COVERED OR REMOVED AS DIRECTED BY THE ENGINEER.

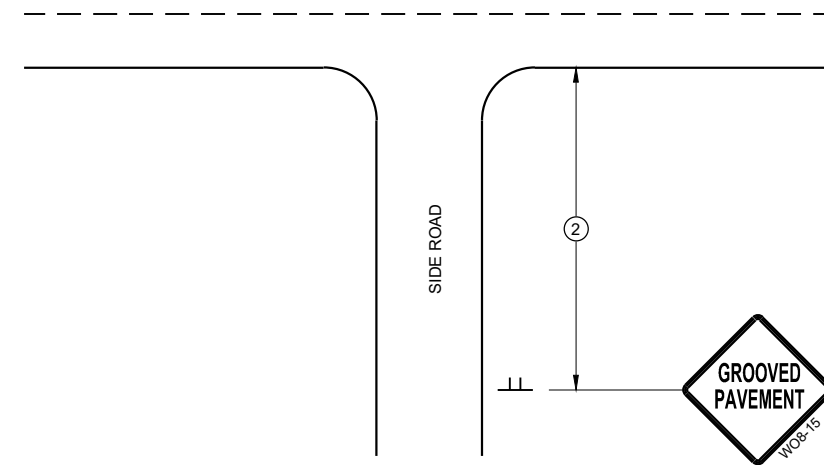
SEE 15C34 FOR ADDITIONAL TRAFFIC CONTROL SIGNING WHEN CENTERLINE PAVEMENT MAKINGS ARE MISSING. 'DO NOT PASS' SIGNS MUST BE INSTALLED ON THE SAME DAY AS MILLING OPERATIONS.

- ① PLACE SIGNS 350' IN ADVANCE OF MILLED SURFACES AND AT 1 MILE INTERVALS, OR AS DIRECTED BY THE ENGINEER.
- ② PLACE SIGN 200' MIN. FROM INTERSECTION AND 200' MIN. AFTER ADVANCE WARNING SIGN SHOWN IN SDD 15C04.

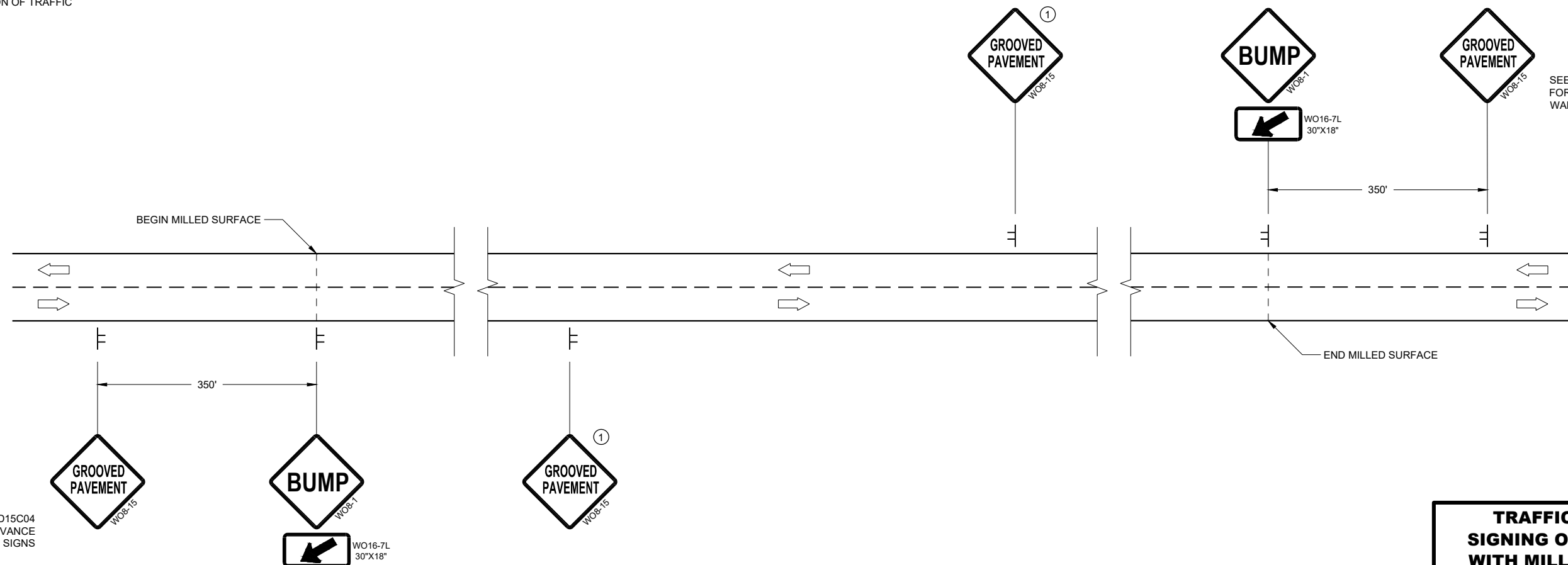
**LEGEND**

⊥ SIGN ON TEMPORARY SUPPORT

⇨ DIRECTION OF TRAFFIC



**TYPICAL SIDE ROAD APPROACH SIGN DETAIL**



SEE SDD15C04 FOR ADVANCE WARNING SIGNS

SEE SDD15C04 FOR ADVANCE WARNING SIGNS

**DETAIL FOR SIGNING ON MILLED SURFACES**

**TRAFFIC CONTROL, SIGNING ON ROADWAYS WITH MILLED SURFACES**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
February 2020 /S/ Andrew Heidtke  
DATE WORK ZONE ENGINEER

FHWA

**GENERAL NOTES**

DRAWING NOT TO SCALE. ALL SIGNS AND POSTS ON THIS SHEET SHALL BE PAID FOR WITH 'TRAFFIC CONTROL SIGNS' BID ITEM. ALL SIDE ROADS WHICH ARE UNDER CONSTRUCTION OF CURB AND GUTTER AND/OR GRADING SHALL BE ADEQUATELY SIGNED.

ALL SIGNS AND DEVICES SHALL BE IN CONFORMANCE WITH THE WISCONSIN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD). SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISDOT STANDARD SIGN PLATES.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THAT THE BACKGROUND IS ORANGE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

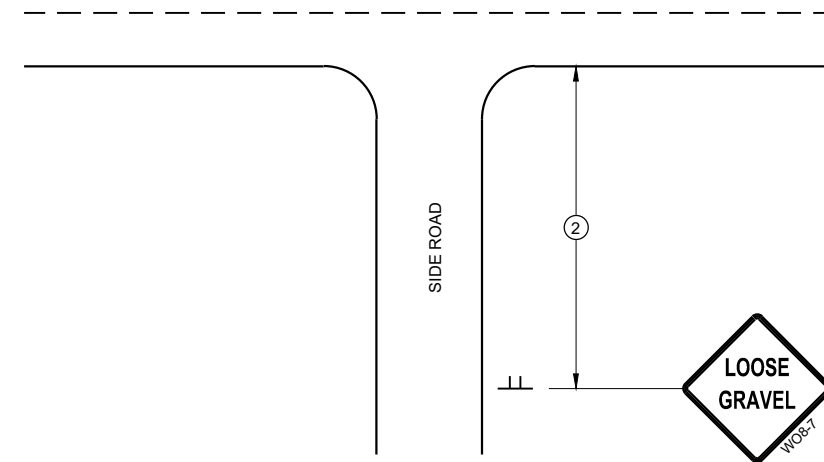
ALL SIGNS INAPPROPRIATE TO THE STATUS OF THE CONTROL ZONE, INCLUDING PRE-EXISTING SIGNS IN THE VICINITY, SHALL BE COVERED OR REMOVED AS DIRECTED BY THE ENGINEER.

SEE 15C34 FOR ADDITIONAL TRAFFIC CONTROL SIGNING WHEN CENTERLINE PAVEMENT MAKINGS ARE MISSING. 'DO NOT PASS' SIGNS MUST BE INSTALLED ON THE SAME DAY AS MILLING OPERATIONS.

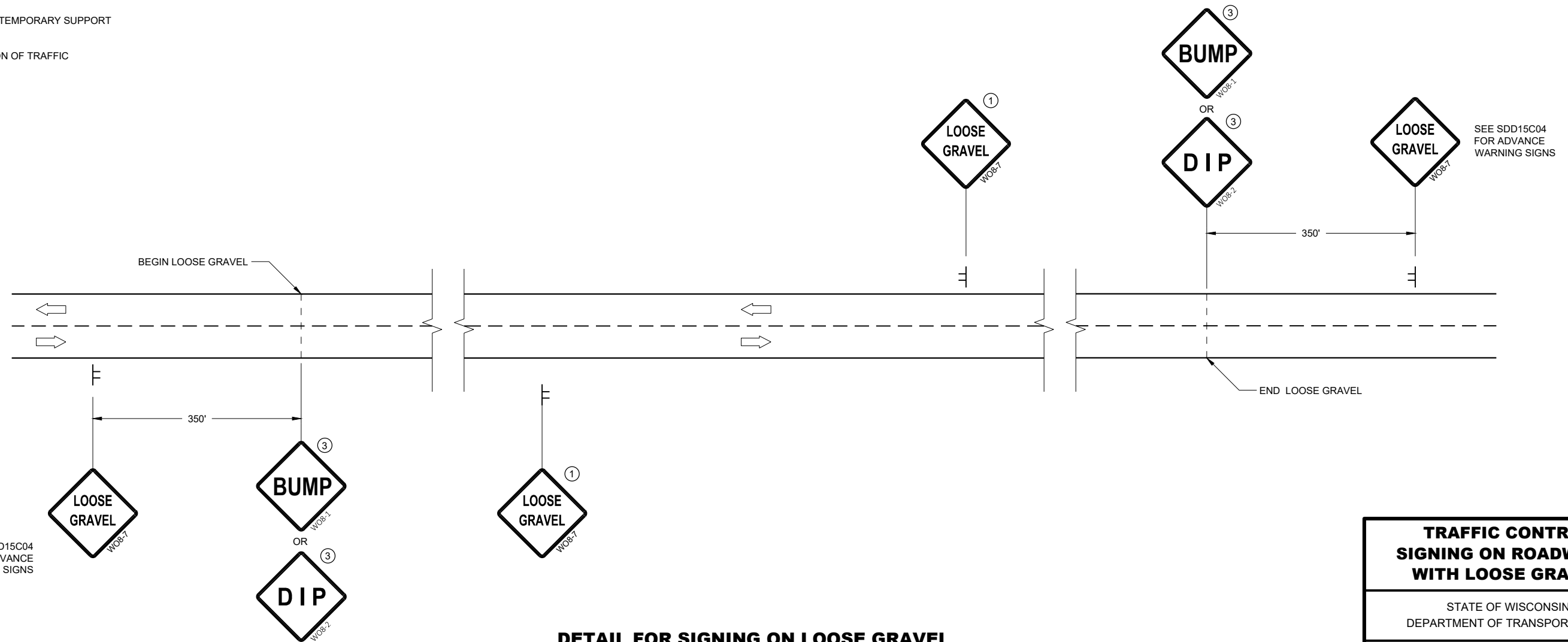
- ① PLACE SIGNS 350' IN ADVANCE OF CHIP SEALED OR LOOSE GRAVEL SURFACES AND AT 1 MILE INTERVALS, OR AS DIRECTED BY THE ENGINEER.
- ② PLACE SIGN 200' MIN. FROM INTERSECTION AND 200' MIN. AFTER ADVANCE WARNING SIGN SHOWN IN SDD 15C04.
- ③ ADD WO8-1 OR WO8-2 SIGN WHEN THE CONDITION IS PRESENT.

**LEGEND**

- ⊥ SIGN ON TEMPORARY SUPPORT
- ➡ DIRECTION OF TRAFFIC



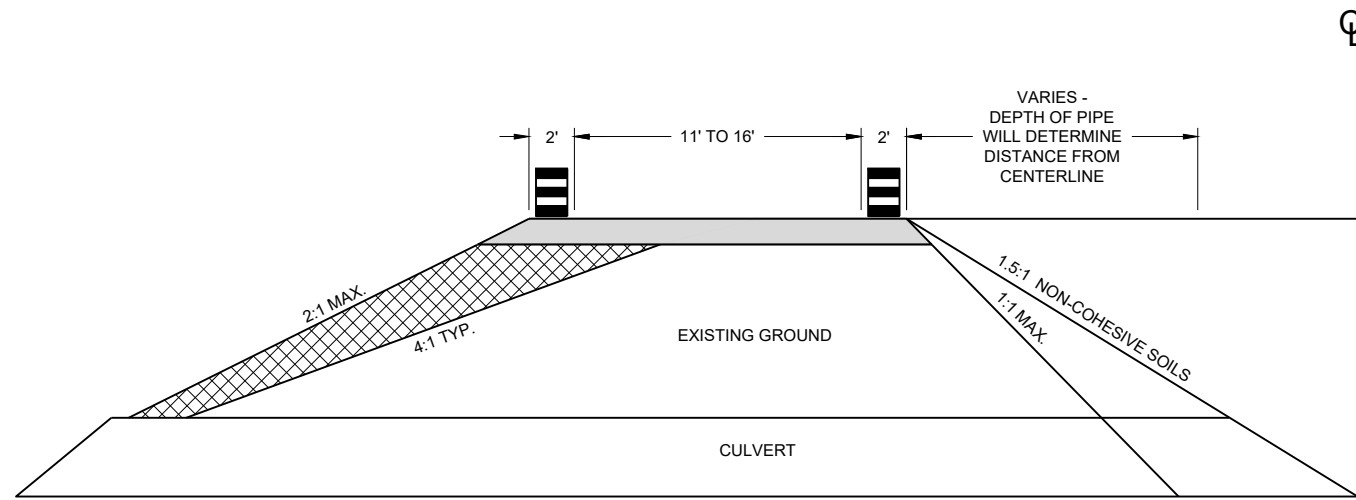
**TYPICAL SIDE ROAD APPROACH SIGN DETAIL**



**DETAIL FOR SIGNING ON LOOSE GRAVEL OR CHIP SEALED SURFACES**

<b>TRAFFIC CONTROL SIGNING ON ROADWAYS WITH LOOSE GRAVEL</b>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED February 2021 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	





**CROSS SECTION**

**GENERAL NOTES**

USE 1:1 FOR COHESIVE CLAYS AND SILTS, LOAMS, SANDY CLAYS AND ANGULAR GRAVEL SOILS.  
 USE 1.5:1 FOR NON-COHESIVE SOILS.

THE TAPER SHOULD EXTEND ACROSS THE SHOULDER UNLESS DOING SO WOULD GREATLY CONFLICT WITH THE WORK OPERATION.






ALL LANE CLOSURE SIGNS SHALL BE REMOVED OR COVERED AND ALL DEVICES REMOVED BEYOND THE SHOULDER WHEN WORK IS NOT IN PROGRESS AND THE LANE IS RESTORED TO A SAFE OPERATING CONDITION.

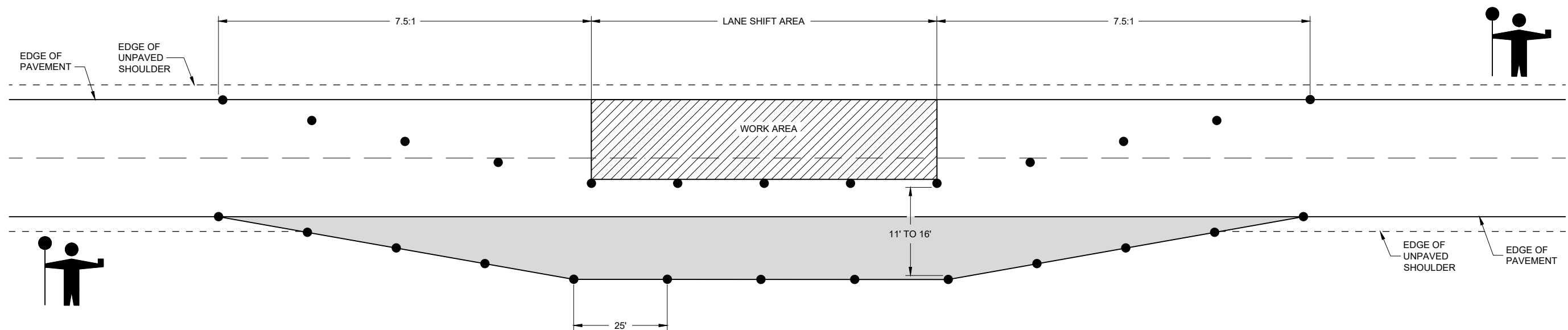
CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

USE WITH SDD 15C12 "TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATIONS"

USE WITH SDD 15D45 "SIGNING ON ROADWAYS WITH LOOSE GRAVEL"

**LEGEND**

-  DRUM WITHOUT WARNING LIGHT
-  6" BASE AGGREGATE DENSE 1 1/2" - INCIDENTAL TO LANE SHIFT ITEM
-  FILL - INCIDENTAL TO LANE SHIFT ITEM
-  WORK AREA
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF



**LANE SHIFT IN FLAGGING OPERATION**

**TRAFFIC CONTROL,  
 TEMPORARY LANE SHIFT  
 DURING CULVERT WORK**

STATE OF WISCONSIN  
 DEPARTMENT OF TRANSPORTATION

APPROVED  
 February 2021 /S/ Andrew Heidtke  
 DATE WORK ZONE ENGINEER

FHWA





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SDD 15D48 - 01

SDD 15D48 - 01

**LEGEND**

- V1 WORK VEHICLE
- V2 SHADOW VEHICLE
-  TRUCK MOUNTED ATTENUATOR (TMA)
-  FLASHING ARROW PANEL (CAUTION)
-  WORK AREA
-  DIRECTION OF TRAFFIC

POSTED SPEED PRIOR TO WORK STARTING (MPH)	DECISION SIGHT DISTANCE (D)
0 - 25	550'
30	550'
35	700'
40	700'
45	900'
50	900'
55	1200'

**GENERAL NOTES**

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

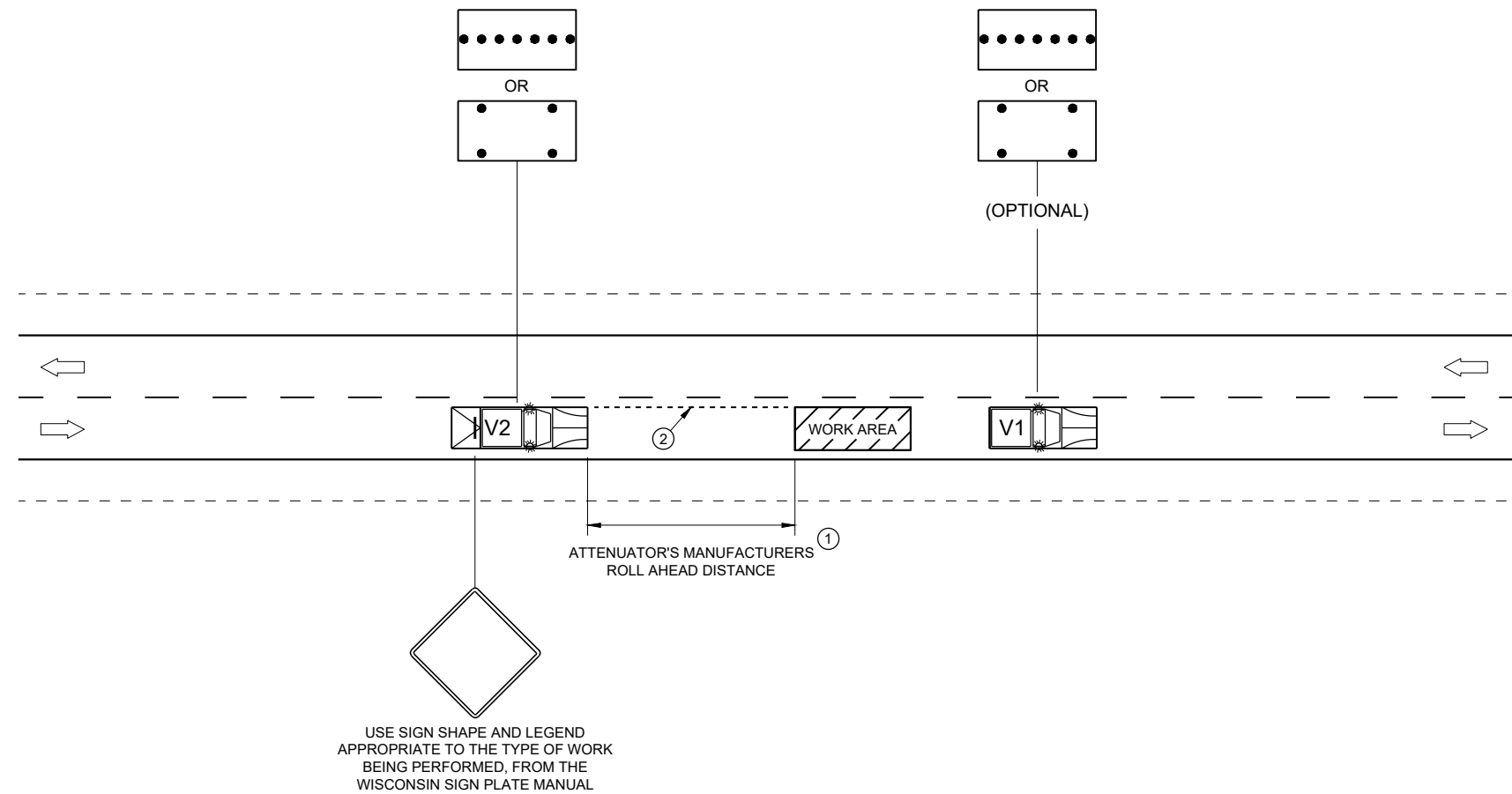
MOBILE IS WORK THAT MOVES CONTINUOUSLY OR MOVES AT LEAST THE DECISION SIGHT DISTANCE EVERY 15 MINUTES.

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

ALL ARROW PANELS SHALL BE REAR FACING, TYPE "B" OR "C", AND DISPLAYING THE FLASHING CAUTION MODE. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

USE AN ATTENUATOR ON THE REARMOST VEHICLE THAT BLOCKS ALL OR PART OF THE TRAFFIC LANE.

- ① DISTANCE BETWEEN VEHICLES MAY INCREASE FROM THE ATTENUATOR'S ROLL AHEAD BASED ON TERRAIN, SIGHT DISTANCE, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.
- ② ALIGN LEFT SIDE OF SHADOW VEHICLE WITH EDGE OF WORK AREA.



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SDD 15D51 - 01

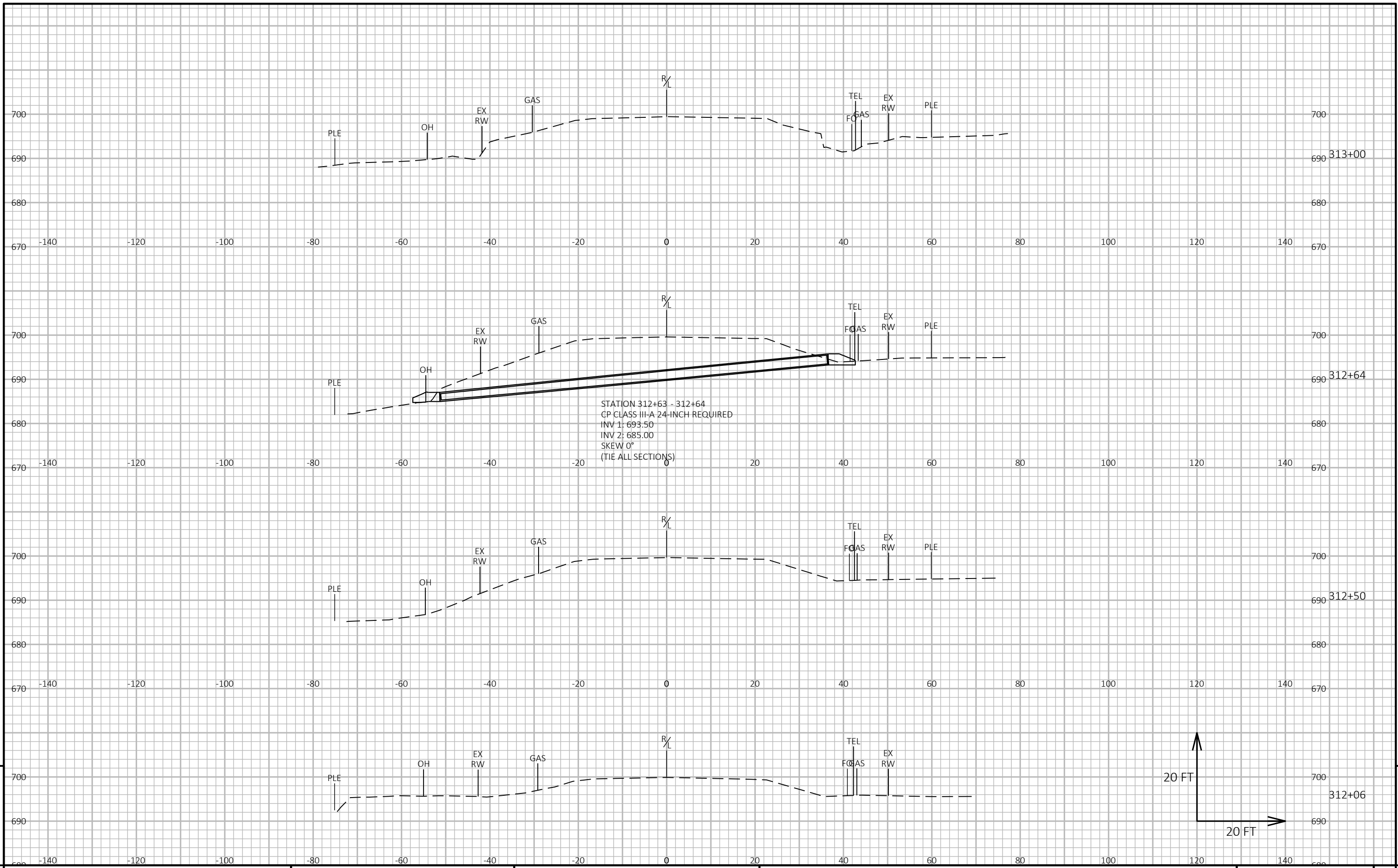
SDD 15D51 - 01

**TRAFFIC CONTROL,  
MOBILE OPERATIONS ON  
AN UNDIVIDED ROADWAY**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
February 2021 DATE /S/ Andrew Heidtke  
STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER

FHWA



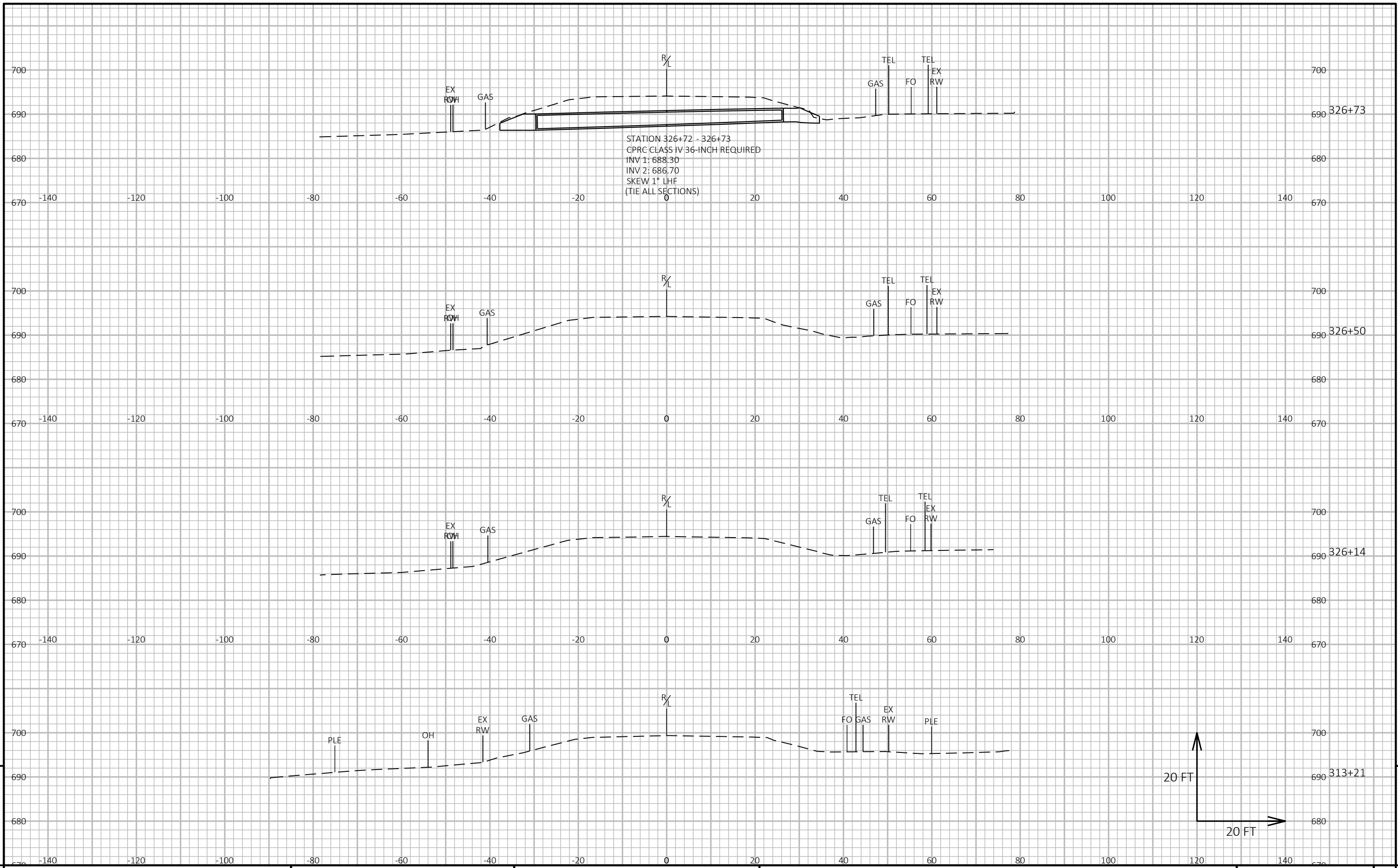
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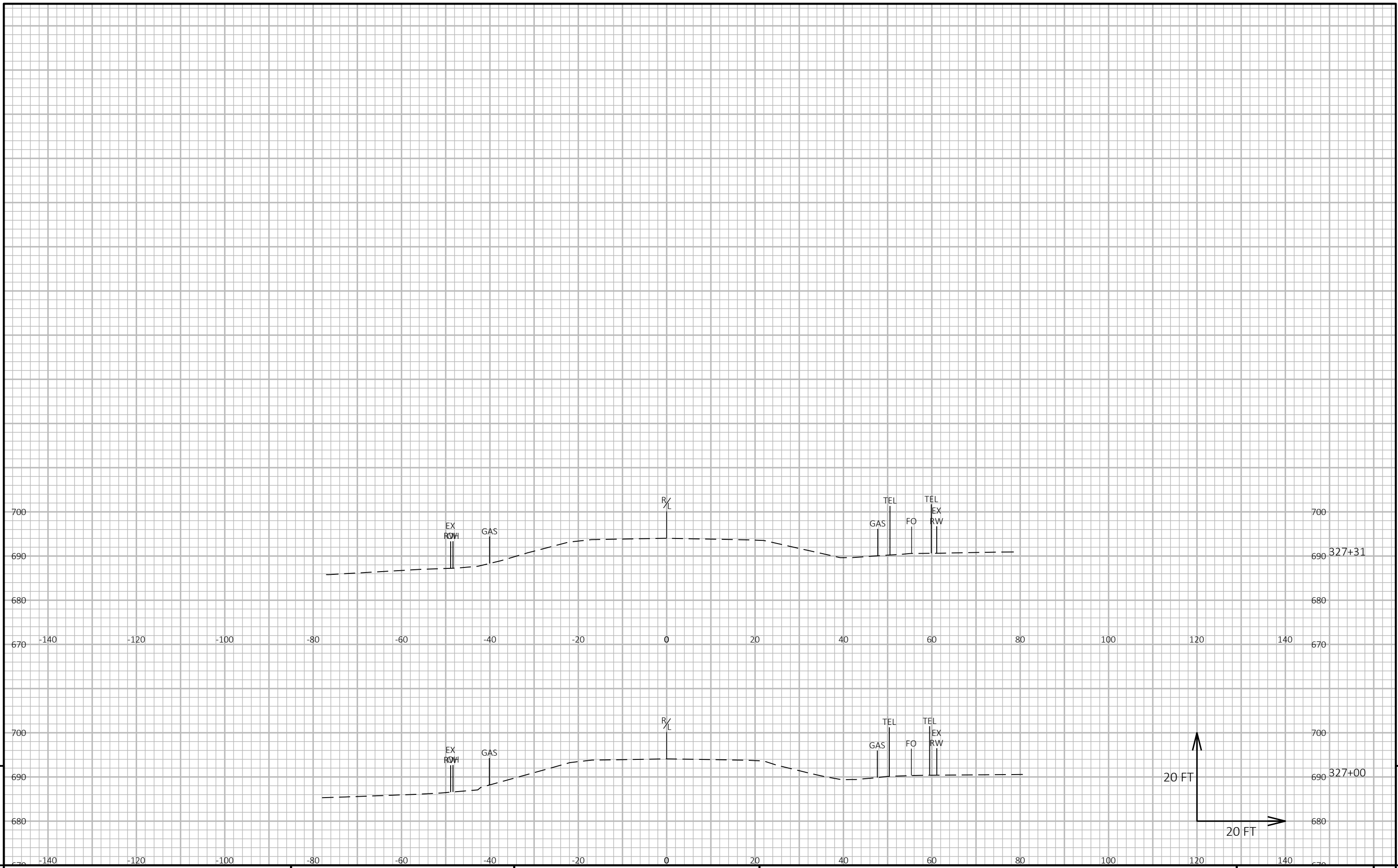
PROJECT NO: 7140-00-70      HWY: STH 35      COUNTY: TREMPEALEAU      CROSS SECTIONS: STH 35 CULVERT REPLACEMENTS      SHEET      E

FILE NAME: N:\PDS\C3D\71400000\SHEETSPLAN\090201-XS.DWG      PLOT DATE: 8/22/2022 2:13 PM      PLOT BY: CORY IHDE      PLOT NAME:      PLOT SCALE: 1 IN:20 FT HORZ. / 1 IN:20 FT VERT.      WISDOT/CADD SHEET 49

LAYOUT NAME - 01



PROJECT NO: 7140-00-70	HWY: STH 35	COUNTY: TREMPLEALEU	CROSS SECTIONS: STH 35 CULVERT REPLACEMENTS	SHEET	<b>9</b>
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PROJECT NO: 7140-00-70	HWY: STH 35	COUNTY: TREMPPEALEAU	CROSS SECTIONS: STH 35 CULVERT REPLACEMENTS	SHEET	<b>E</b>
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## ***Wisconsin Department of Transportation***

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