

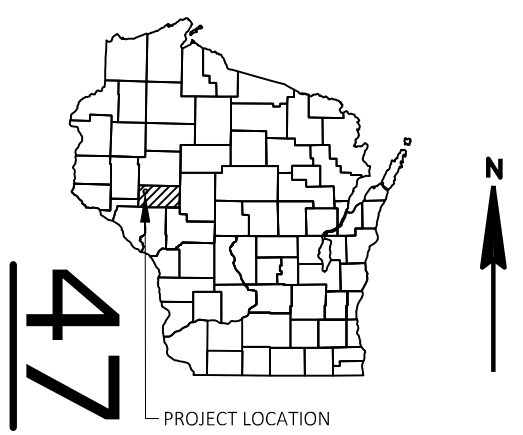
EAU
 PROJECT ID: 7995-02-65
 WITH: N/A
 COUNTY: EAU CLAIRE

FEBRUARY 2023

ORDER OF SHEETS

Section No.	Title
1	Title
2	Typical Sections and Details
3	Estimate of Quantities
3	Miscellaneous Quantities
4	Right of Way Plat
5	Plan and Profile
6	Standard Detail Drawings
7	Sign Plates
8	Structure Plans
9	Computer Earthwork Data
9	Cross Sections

TOTAL SHEETS = 88



DESIGN DESIGNATION

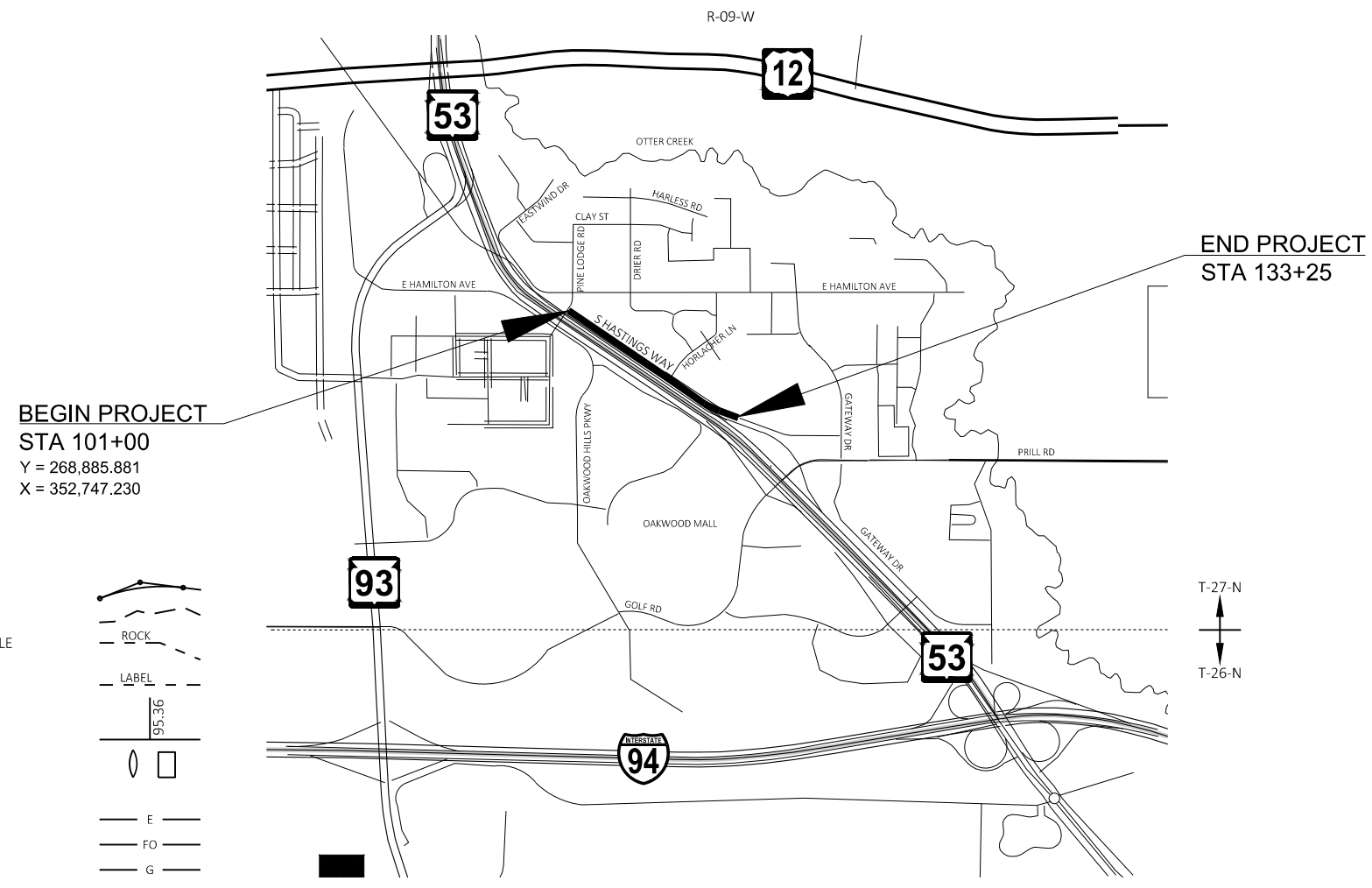
A.A.D.T. (2023)	=	5,300
A.A.D.T. (2043)	=	5,850
D.H.V.	=	585
D.D.	=	50/50
T.	=	4%
DESIGN SPEED	=	40 MPH
ESALS	=	610,000

CONVENTIONAL SYMBOLS

PLAN	PROFILE
CORPORATE LIMITS	GRADE LINE
PROPERTY LINE	ORIGINAL GROUND
LOT LINE	MARSH OR ROCK PROFILE (To be noted as such)
LIMITED HIGHWAY EASEMENT	SPECIAL DITCH
EXISTING RIGHT OF WAY	GRADE ELEVATION
PROPOSED OR NEW R/W LINE	CULVERT (Profile View)
SLOPE INTERCEPT	UTILITIES
REFERENCE LINE	ELECTRIC
EXISTING CULVERT	FIBER OPTIC
PROPOSED CULVERT (Box or Pipe)	GAS
COMBUSTIBLE FLUIDS	SANITARY SEWER
MARSH AREA	STORM SEWER
WOODED OR SHRUB AREA	TELEPHONE
	WATER
	UTILITY PEDESTAL
	POWER POLE
	TELEPHONE POLE

STATE OF WISCONSIN
 DEPARTMENT OF TRANSPORTATION
 PLAN OF PROPOSED IMPROVEMENT
C EAU CLAIRE, S HASTINGS WAY
 GATEWAY DRIVE TO PINE LODGE ROAD
 LOCAL STREET
 EAU CLAIRE COUNTY

STATE PROJECT NUMBER
7995-02-65



LAYOUT
 SCALE 0 0.5 MI
 TOTAL NET LENGTH OF CENTERLINE = 0.611 MI

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS), EAU CLAIRE COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES. ELEVATIONS ARE REFERENCED TO NAVD 88 (2012). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 12A.

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
7995-02-65	WISC 2023247	1

APPROVED FOR THE CITY OF EAU CLAIRE
 DATE: 10/27/22
 (Signature)

ORIGINAL PLANS PREPARED BY
CORRE
 ENGINEERING
 MADISON | OCONOMOWOC | EAU CLAIRE | GREEN BAY | WITTEMBERG

WISCONSIN PROFESSIONAL ENGINEER
 KEVIN L. MEYER
 E-38809-008
 ELK MOUND WI
 10/27/22

DATE: _____
 (Signature)

STATE OF WISCONSIN
 DEPARTMENT OF TRANSPORTATION

PREPARED BY

Surveyor	AYRES
Designer	CORRE INC
Project Manager	TYLER RONGSTAD, PE
Regional Examiner	TOU YANG, PE
Regional Supervisor	TYLER RONGSTAD, PE

APPROVED FOR THE DEPARTMENT
 DATE: 10/31/2022
 Tyler Rongstad
 (Signature)

E

GENERAL NOTES

WHEN THE QUANTITY OF BASE AGGREGATE OR ASPHALTIC SURFACE IS MEASURED FOR PAYMENT BY THE TON OR CUBIC YARD, THE DEPTH OR THICKNESS OF THE LAYER SHOWN ON THE PLANS IS APPROXIMATE AND THE ACTUAL THICKNESS WILL DEPEND ON THE DISTRIBUTION OF THE MATERIAL AS DIRECTED BY THE ENGINEER.

THE LOCATION OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS IS APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

NO TREES OR SHRUBS ARE TO BE REMOVED WITHOUT THE APPROVAL OF THE ENGINEER.

THE EXACT LOCATION OF THE EROSION CONTROL DEVICES SHALL BE DETERMINED IN THE FIELD.

ALL CURB AND GUTTER RADII, PAVEMENT DIMENSIONS AND STATIONS ARE SHOWN TO THE EDGE OF PAVEMENT UNLESS NOTED OTHERWISE.

ALL SIDE ROAD EARTHWORK QUANTITIES ARE INCLUDED IN MAINLINE EARTHWORK QUANTITIES.

A VERTICAL SAWCUT SHALL BE MADE THROUGH EXISTING DRIVEWAYS AND PAVEMENTS AT REMOVAL LIMITS.

EXCAVATION BELOW SUBGRADE (EBS) IS NOT USED TO BALANCE YARDAGE AND IS NOT SHOWN ON THE CROSS SECTIONS BUT IS MEASURED AND PAID FOR AS COMMON OR ROCK EXCAVATION.

RUNOFF COEFFICIENT TABLE

	HYDROLOGIC SOIL GROUP											
	A			B			C			D		
	SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)		
LAND USE:	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER
ROW CROPS	.08 .22	.16 .30	.22 .38	.12 .26	.20 .34	.27 .44	.15 .30	.24 .37	.33 .50	.19 .34	.28 .41	.38 .56
MEDIAN STRIP-TURF	.19 .24	.20 .26	.24 .30	.19 .25	.22 .28	.26 .33	.20 .26	.23 .30	.30 .37	.20 .27	.25 .32	.30 .40
SIDE SLOPE-TURF			.25 .32			.27 .34			.28 .36			.30 .38
PAVEMENT:												
ASPHALT	.70 - .95											
CONCRETE	.80 - .95											
BRICK	.70 - .80											
DRIVES, WALKS	.75 - .85											
ROOFS	.75 - .95											
GRAVEL ROADS, SHOULDERS	.40 - .60											

TOTAL PROJECT AREA = 4.8 ACRES
TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = 2.6 ACRES

UTILITY CONTACTS

COMMUNICATIONS

AT&T

RICK PODOLAK
304 S DEWEY STREET
4TH FLOOR
EAU CLAIRE, WI 54701
PHONE: (715) 839-5565
EMAIL: RP4514@ATT.COM

SPECTRUM

RYAN LUCKEN
1201 MCCANN DRT
ALTOONA, WI 54720
PHONE: (715) 833-7390
EMAIL: RYAN.LUCKEN@CHARTER.COM

ELECTRICITY

XCEL ENERGY

ROSS AUNA
1400 WESTERN AVENUE
EAU CLAIRE, WI 54702
PHONE: (715) 737-1096
EMAIL: ROSS.A.AUNA@XCELENERGY.COM

ITS EQUIPMENT

WisDOT

DEAN BEEKMAN
433 WEST ST PAUL AVE, SUITE 300
MILWAUKEE, WI 53203
PHONE: (414) 227-2154
EMAIL: DEAN.BEEKMAN@DOT.WI.GOV

SANITARY SEWER

CITY OF EAU CLAIRE

BEN SPANEL
1000 FERRY STREET
EAU CLAIRE, WI 54703
PHONE: (715) 839-6117
EMAIL: BEN.SPANEL@EAUCLAIREWI.GOV

WATER

CITY OF EAU CLAIRE

BEN SPANEL
2711 RIVERVIEW DRIVE
EAU CLAIRE, WI 54703
PHONE: (715) 839-5045
EMAIL: BEN.SPANEL@EAUCLAIREWI.GOV

GAS

XCEL ENERGY

MIKE JOHNSEN
1400 Western Ave
EAU CLAIRE, WI 54702
PHONE: (715) 737-4203
EMAIL: MICHAEL.A.JOHNSEN@XCELENERGY.COM

DIGGERS HOTLINE
Dial **811** or (800)242-8511
www.DiggersHotline.com

WISDOT CONTACT

NORTHWEST REGION
TYLER RONGSTAD, PE
718 WEST CLAIREMONT AVE
EAU CLAIRE, WI 54701-5108
PHONE: (715) 461-0372
EMAIL: TYLER.RONGSTAD@DOT.WI.GOV

CITY CONTACT

CITY OF EAU CLAIRE
LEAH NESS
203 S FARWELL STREET
EAU CLAIRE, WI 54701
PHONE: (715) 839-4934
EMAIL: LEAH.NESS@EAUCLAIREWI.GOV

DNR CONTACT

DNR WEST CENTRAL REGION HEADQUARTERS
LEAH NICOL
1300 WEST CLAIREMONT AVE
EAU CLAIRE, WI 54701
PHONE: (715) 836-6571
EMAIL: LEAH.NICOL@WISCONSIN.GOV

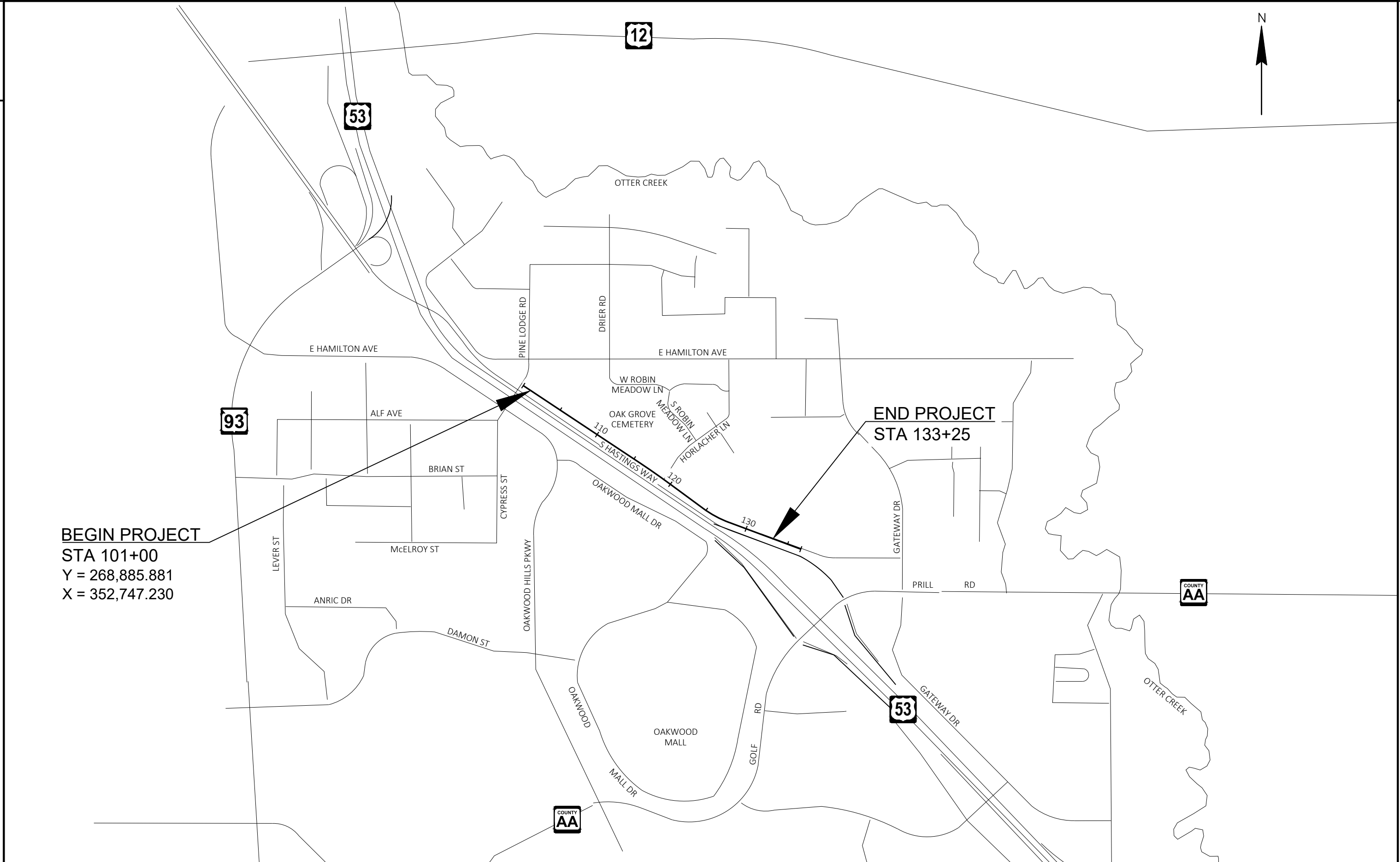
DESIGN CONTACT

CORRE, INC.
KEVIN MEYER
1802 WARDEN ST
EAU CLAIRE, WI 54703
PHONE: (715) 299-1894
EMAIL: KMEYER@CORREINC.COM

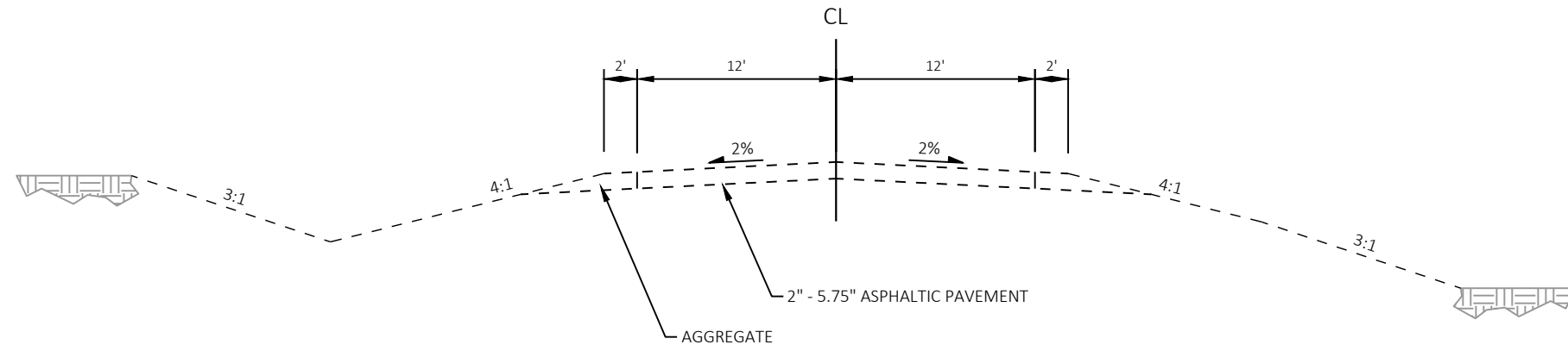


BEGIN PROJECT
STA 101+00
 Y = 268,885.881
 X = 352,747.230

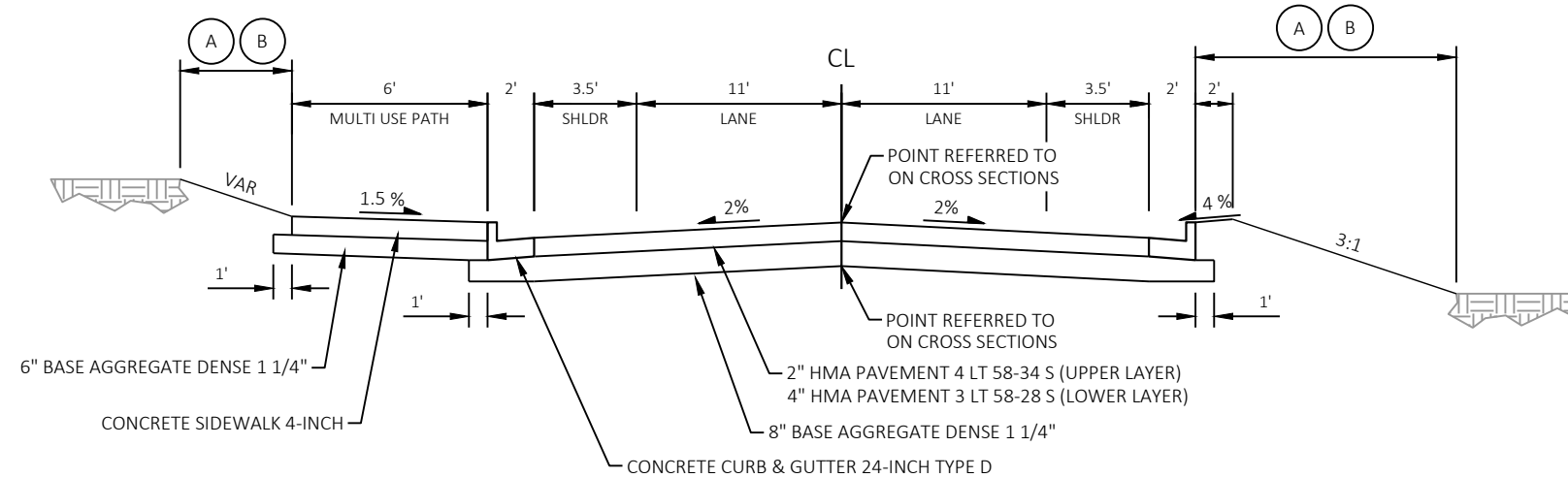
END PROJECT
STA 133+25



PROJECT NO: 7995-02-65	HWY: S HASTINGS WAY	COUNTY: EAU CLAIRE	PROJECT OVERVIEW	SHEET	E
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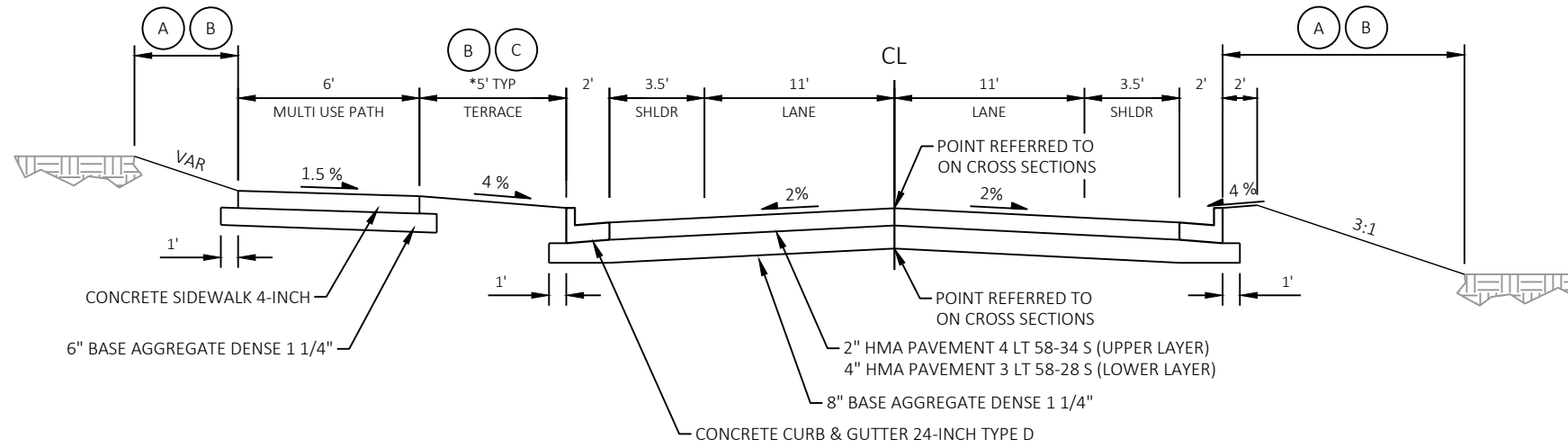
EXISTING TYPICAL SECTION - S HASTINGS WAY
STA 101+00 - STA 133+25



FINISHED TYPICAL SECTION - S HASTINGS WAY
STA 101+00 - STA 101+77

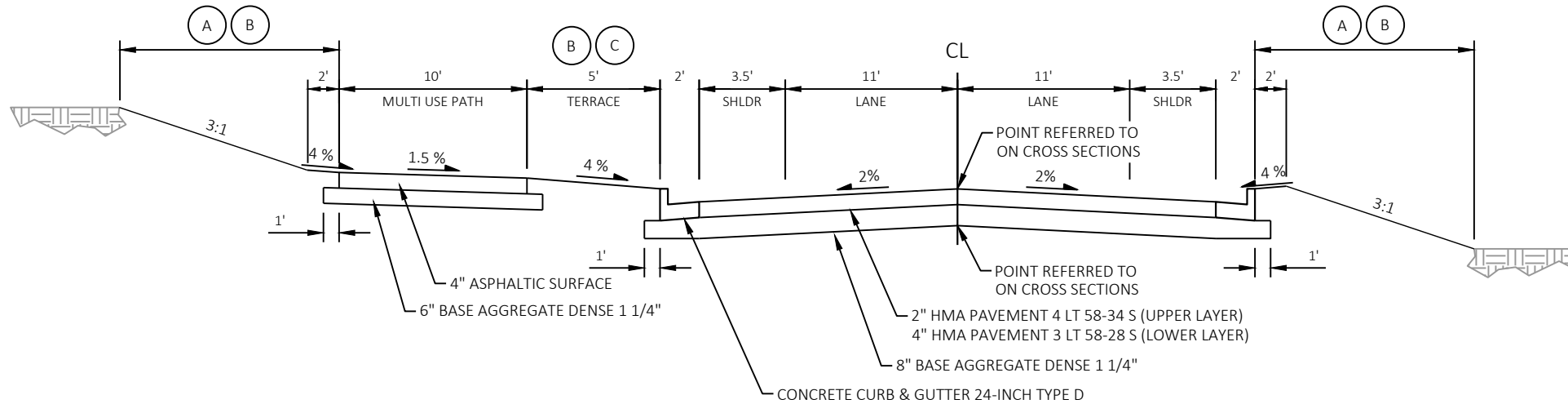
- (A) SALVAGED TOPSOIL & MULCH
- (B) SEEDING MIXTURE NO. 10 & FERTILIZER TYPE B
- (C) TOPSOIL & EROSION MAT

*VARIES 0'-5'



FINISHED TYPICAL SECTION - S HASTINGS WAY

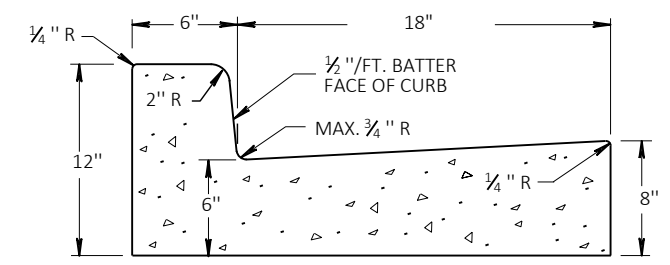
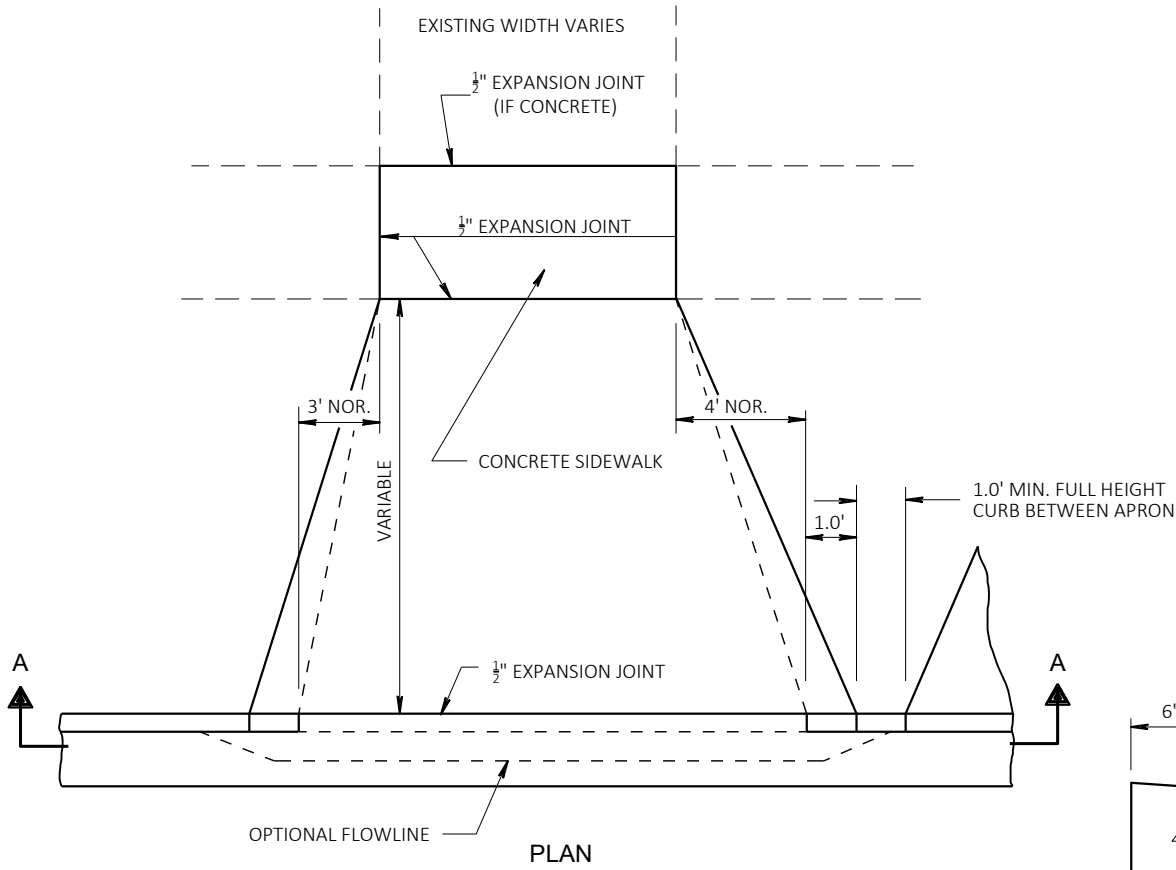
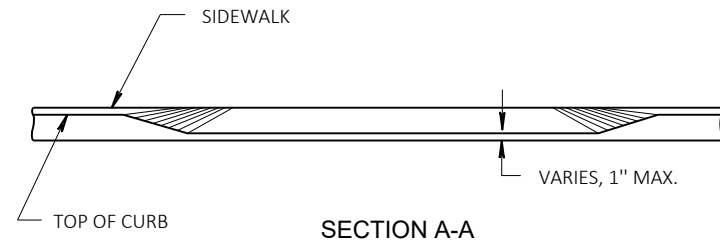
STA 101+77 - STA 103+15



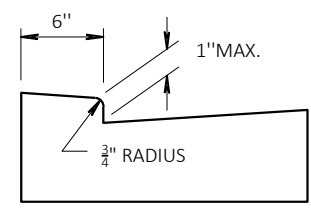
FINISHED TYPICAL SECTION - S HASTINGS WAY

STA 103+15 - STA 133+25

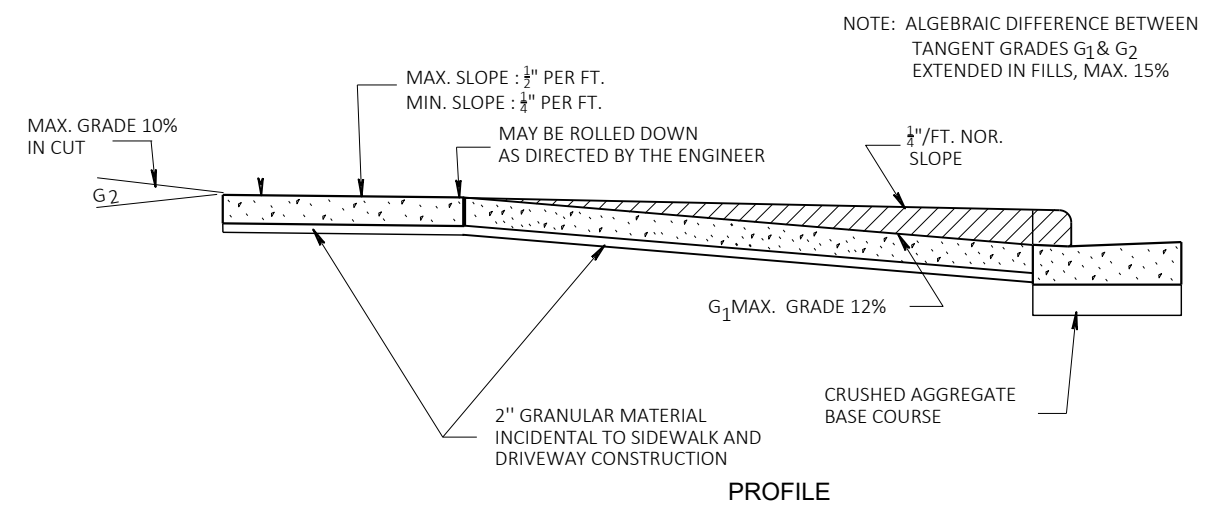
- (A) SALVAGED TOPSOIL & MULCH
- (B) SEEDING MIXTURE NO. 10 & FERTILIZER TYPE B
- (C) TOPSOIL & EROSION MAT



CONCRETE CURB & GUTTER 24-INCH TYPE D

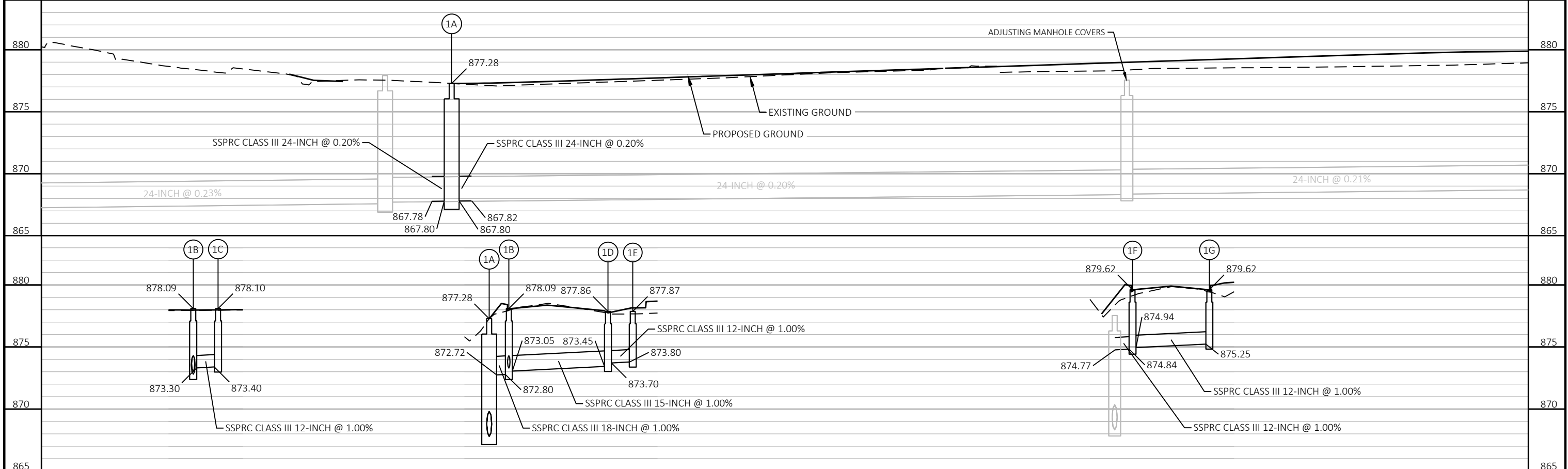
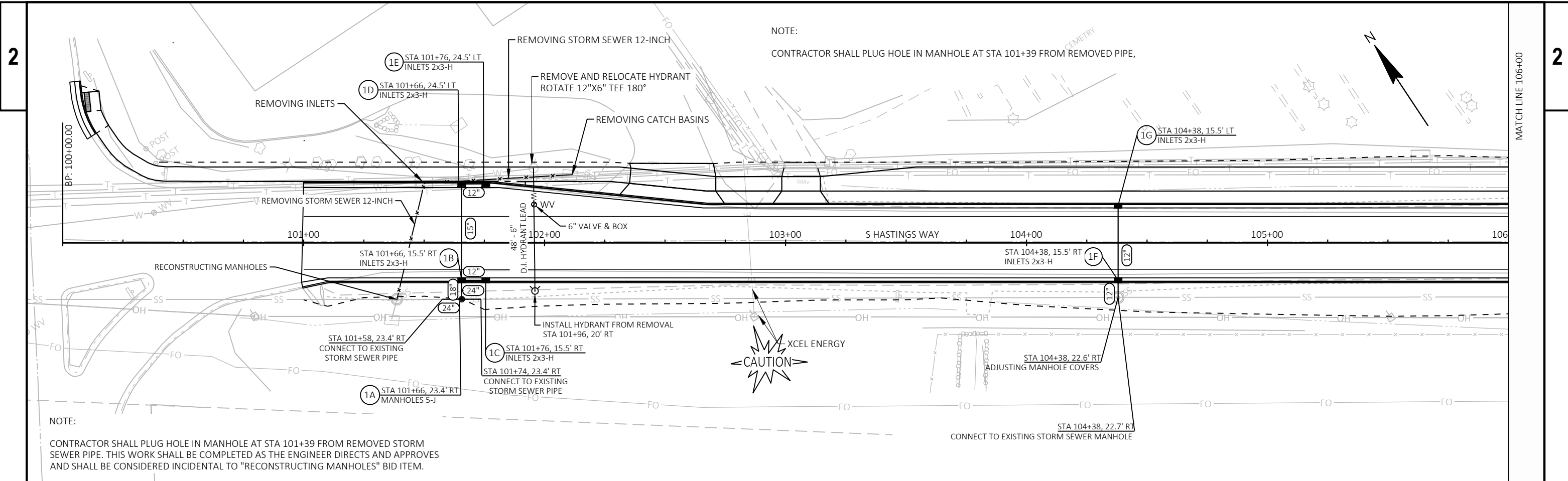


OPTIONAL METHOD FOR CONCRETE GUTTER



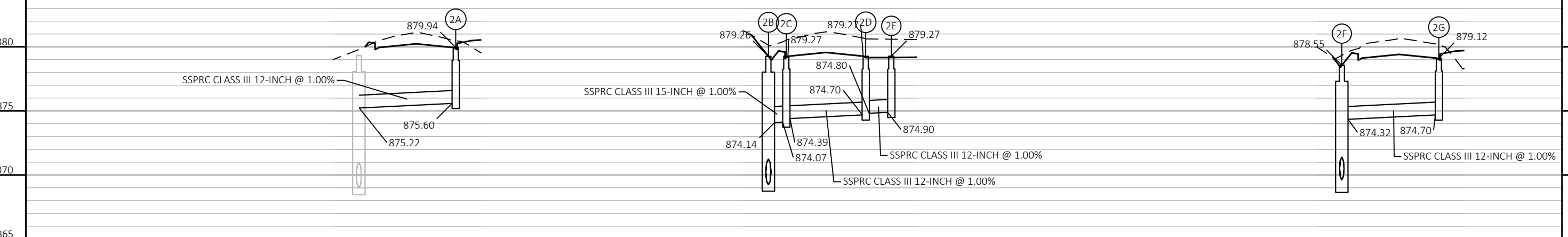
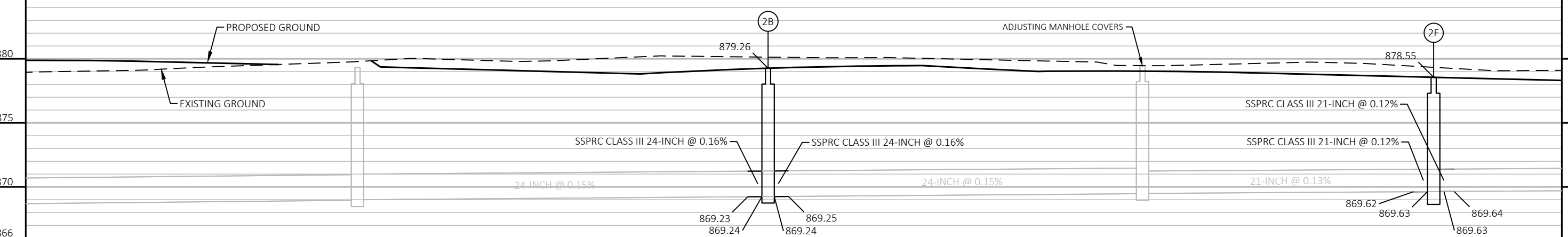
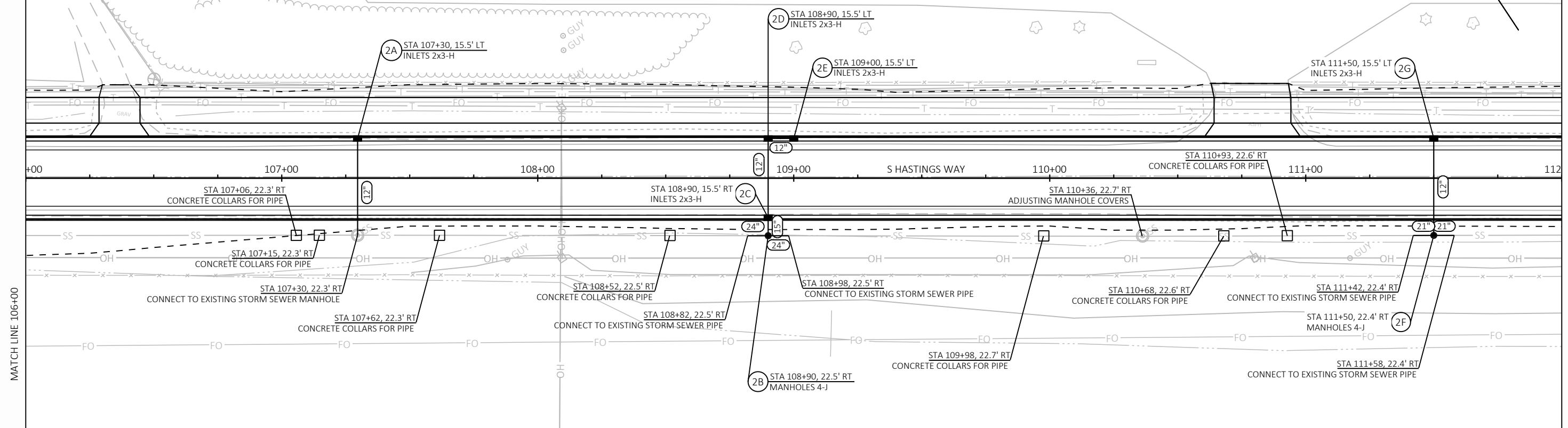
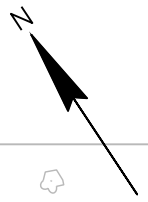
URBAN DRIVEWAY DETAIL

PROJECT NO: 7995-02-65	HWY: S HASTINGS WAY	COUNTY: EAU CLAIRE	CONSTRUCTION DETAILS	SHEET	E
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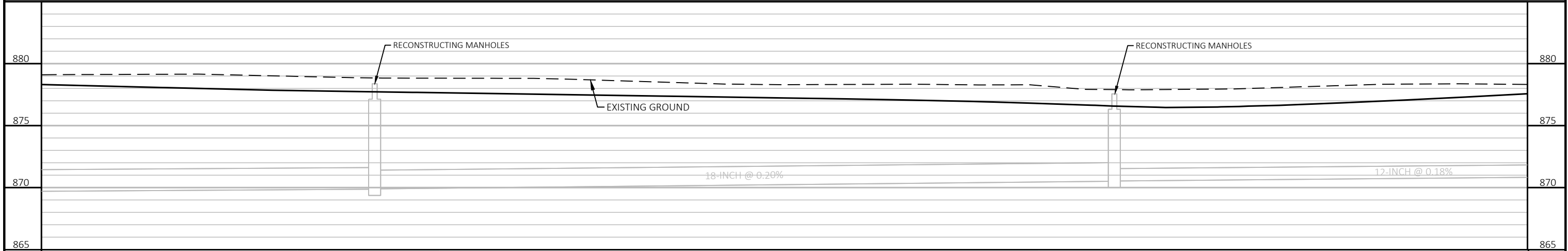
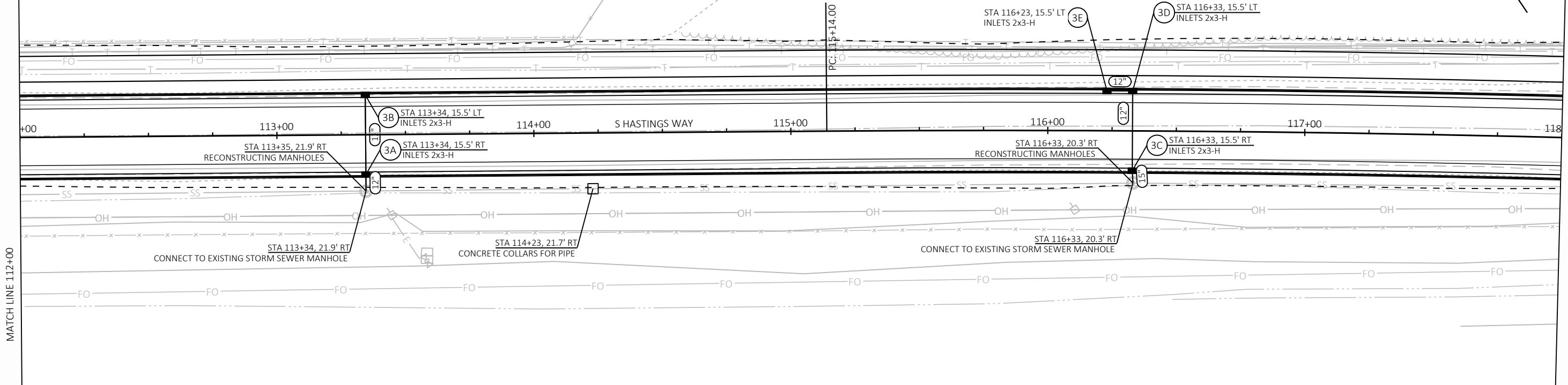
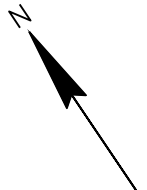


PROJECT NO: 7995-02-65	HWY: S HASTINGS WAY	COUNTY: EAU CLAIRE	STORM SEWER PLAN	SHEET	E
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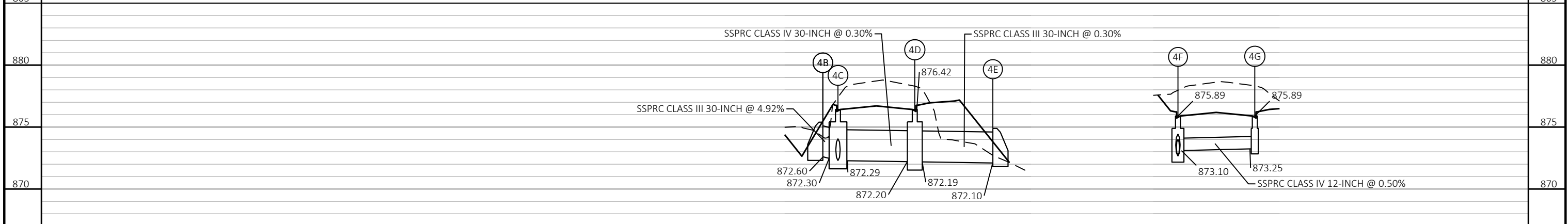
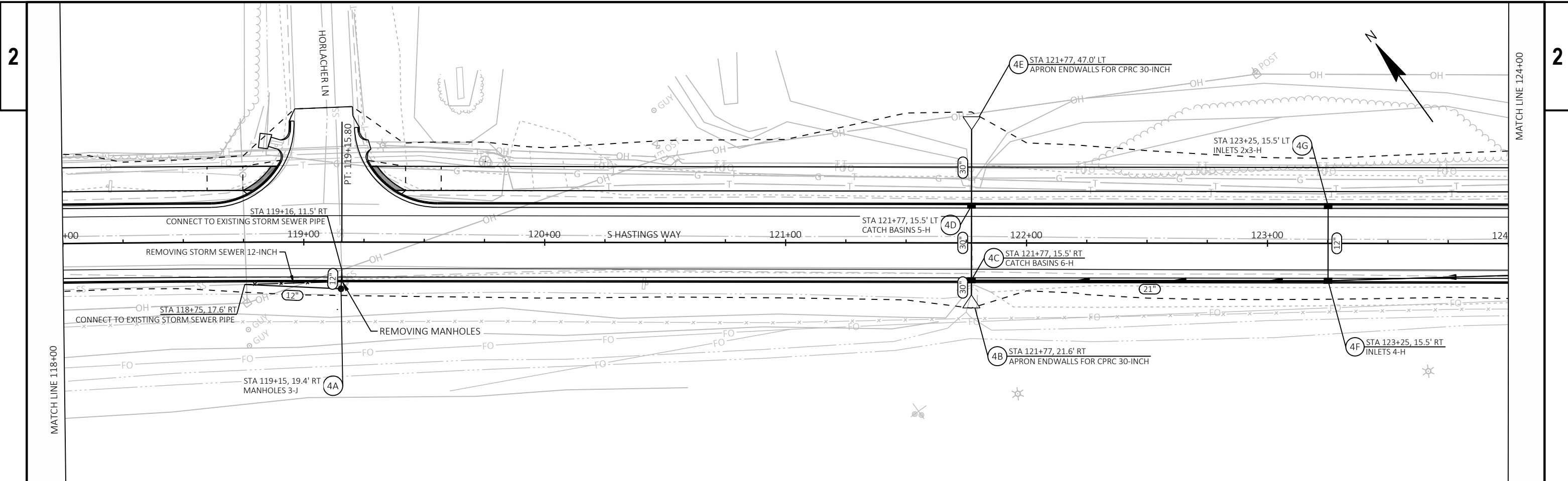
NOTE:
 EXCAVATION AND BACKFILL FOR CONCRETE COLLARS SHALL BE CONSIDERED INCIDENTAL TO BID ITEM "CONCRETE COLLARS FOR PIPE."



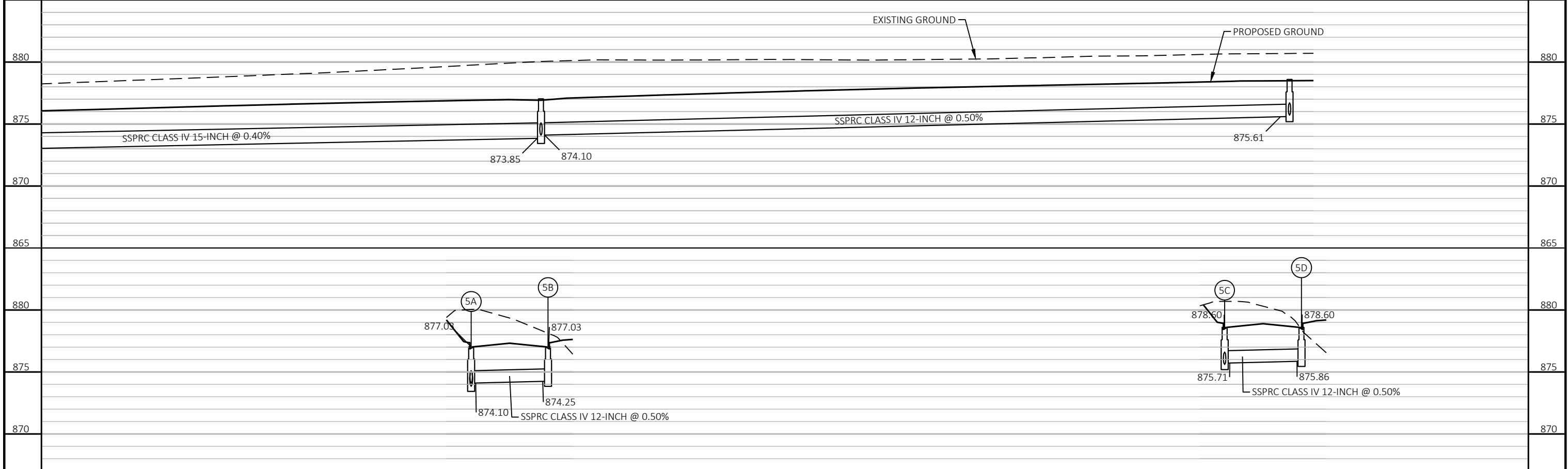
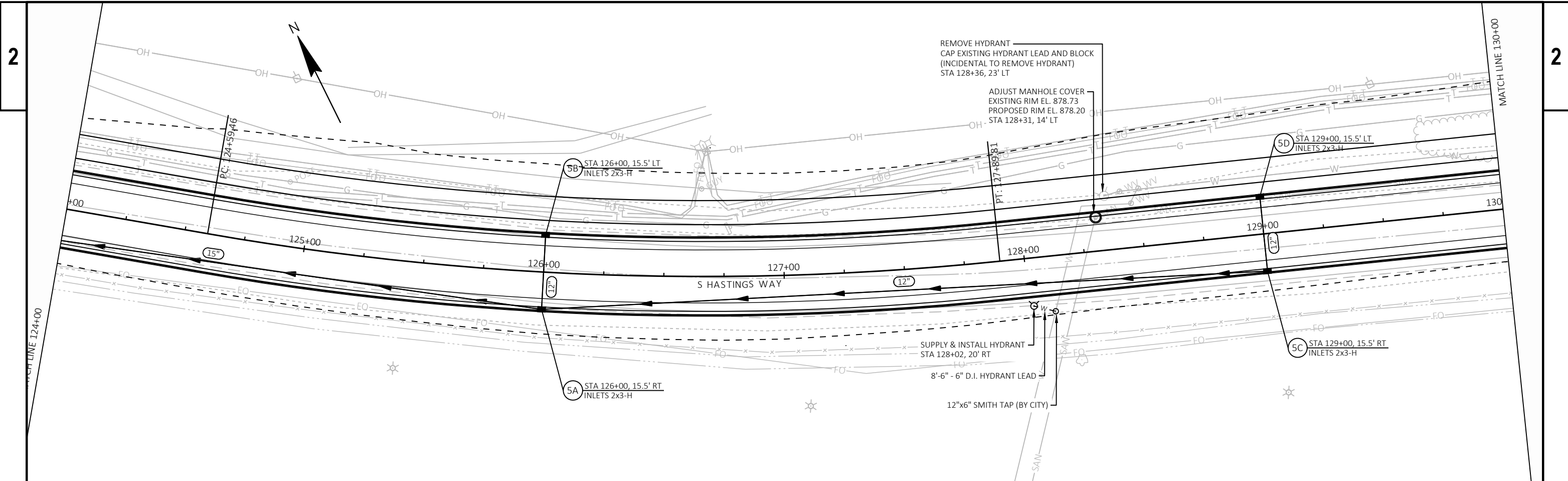
NOTE:
EXCAVATION AND BACKFILL FOR CONCRETE COLLARS SHALL BE
CONSIDERED INCIDENTAL TO BID ITEM "CONCRETE COLLARS FOR PIPE."



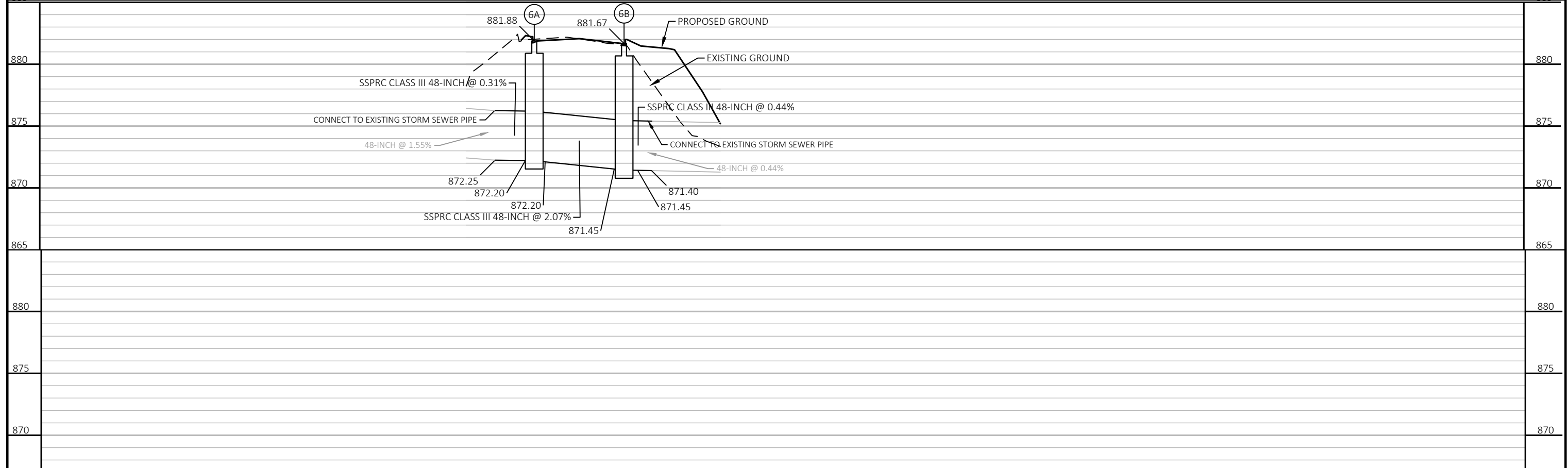
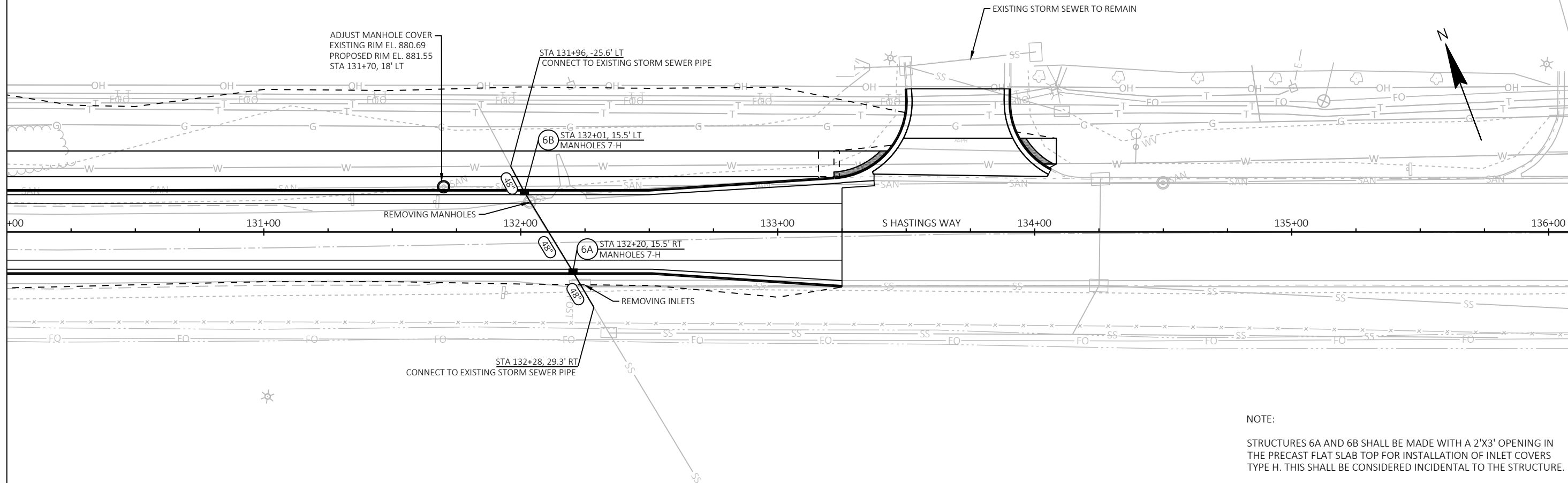
PROJECT NO: 7995-02-65	HWY: S HASTINGS WAY	COUNTY: EAU CLAIRE	STORM SEWER PLAN	SHEET	E
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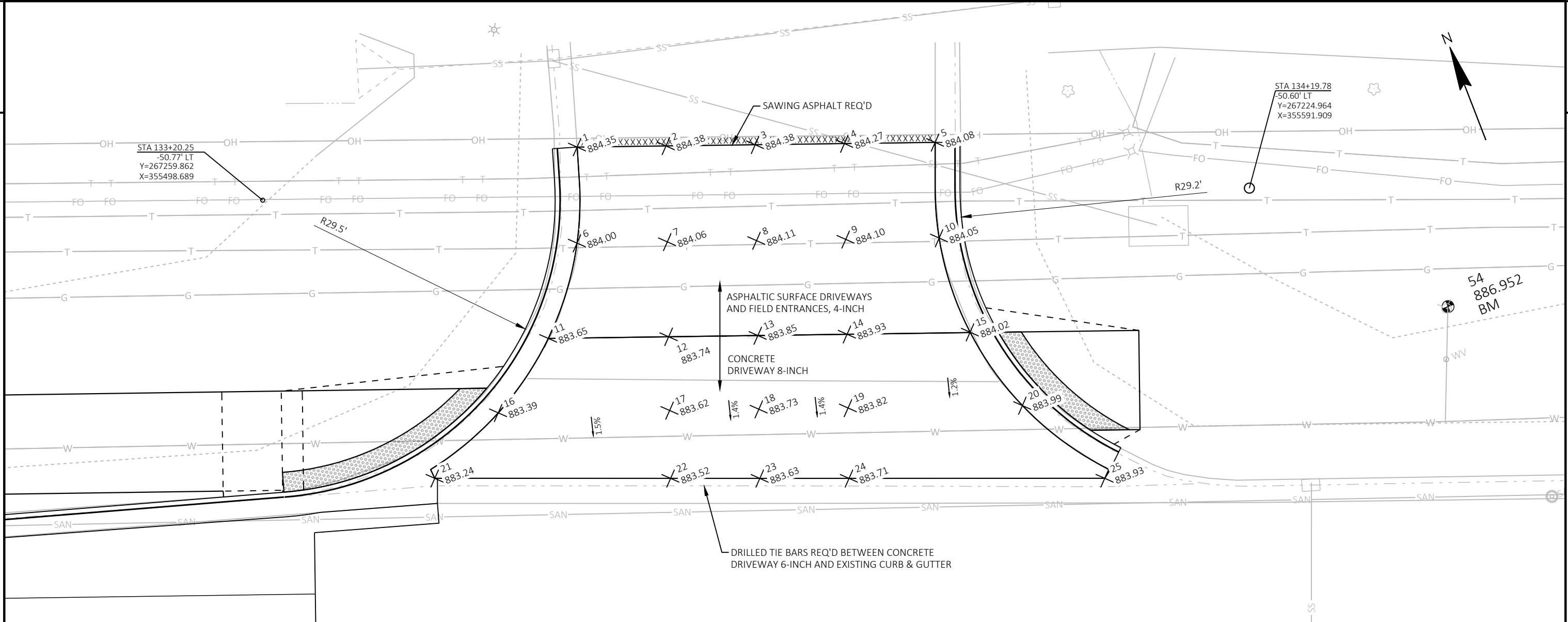
PROJECT NO: 7995-02-65	HWY: S HASTINGS WAY	COUNTY: EAU CLAIRE	STORM SEWER PLAN	SHEET	E
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PROJECT NO: 7995-02-65	HWY: S HASTINGS WAY	COUNTY: EAU CLAIRE	STORM SEWER PLAN	SHEET	E
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PROJECT NO: 7995-02-65	HWY: S HASTINGS WAY	COUNTY: EAU CLAIRE	STORM SEWER PLAN	SHEET	E
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STA 133+20.25
-50.77' LT
Y=267259.862
X=355498.689

STA 134+19.78
-50.60' LT
Y=267224.964
X=355591.909

STATION & OFFSET TABLE				
POINT	STATION	OFFSET	Y COORDS	X COORDS
1	133+52.04	55.68 LT	267253.368	355530.195
2	133+61.00	55.67 LT	267250.233	355538.591
3	133+70.00	55.66 LT	267247.085	355547.023
4	133+79.00	55.66 LT	267243.937	355555.454
5	133+88.16	55.65 LT	267240.735	355564.032
6	133+51.89	46.00 LT	267244.348	355526.683
7	133+61.00	46.00 LT	267241.170	355535.216
8	133+70.00	46.00 LT	267238.029	355543.650
9	133+79.00	46.00 LT	267234.888	355552.084
10	133+88.42	46.00 LT	267231.599	355560.916
11	133+48.86	36.43 LT	267236.440	355520.501
12	133+61.00	36.43 LT	267232.204	355531.877
13	133+70.00	36.43 LT	267229.063	355540.311

STATION & OFFSET TABLE				
POINT	STATION	OFFSET	Y COORDS	X COORDS
14	133+79.00	36.43 LT	267225.922	355548.745
15	133+91.41	36.47 LT	267221.625	355560.389
16	133+43.71	29.00 LT	267231.273	355513.078
17	133+61.00	29.00 LT	267225.239	355529.283
18	133+70.00	29.00 LT	267222.098	355537.718
19	133+79.00	29.00 LT	267218.957	355546.152
20	133+96.59	29.00 LT	267212.820	355562.633
21	133+37.21	22.49 LT	267227.442	355504.720
22	133+61.00	22.16 LT	267218.824	355526.895
23	133+70.00	22.03 LT	267215.564	355535.284
24	133+79.00	21.90 LT	267212.307	355543.675
25	134+04.79	21.55 LT	267202.976	355567.720

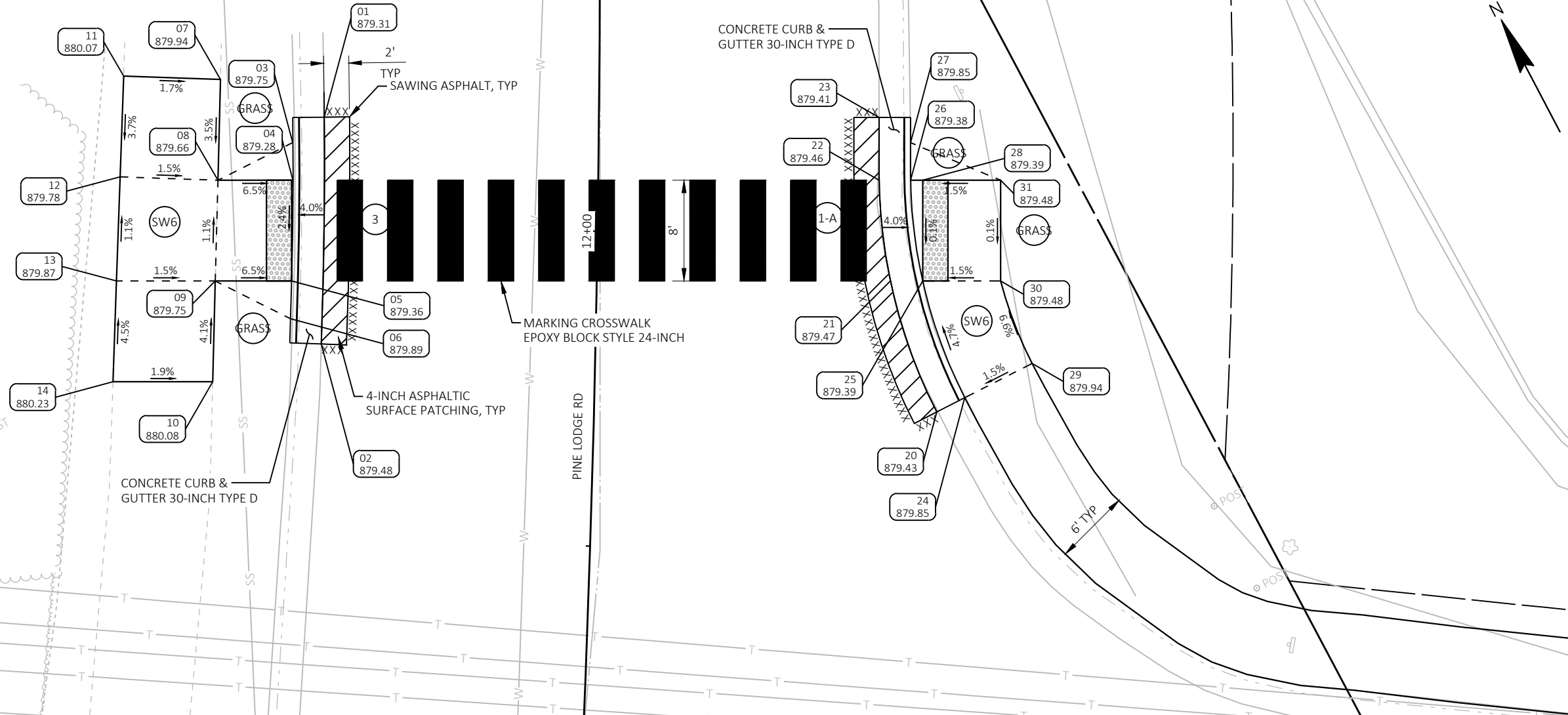
S HASTINGS WAY

134+00

133+00

LEGEND

- (X) CURB RAMP TYPE
- (SW6) CONCRETE SIDEWALK 6-INCH
- (ASPH) ASPHALTIC SURFACE
- (XX
YYY.YYY) POINT NUMBER AND ELEVATION

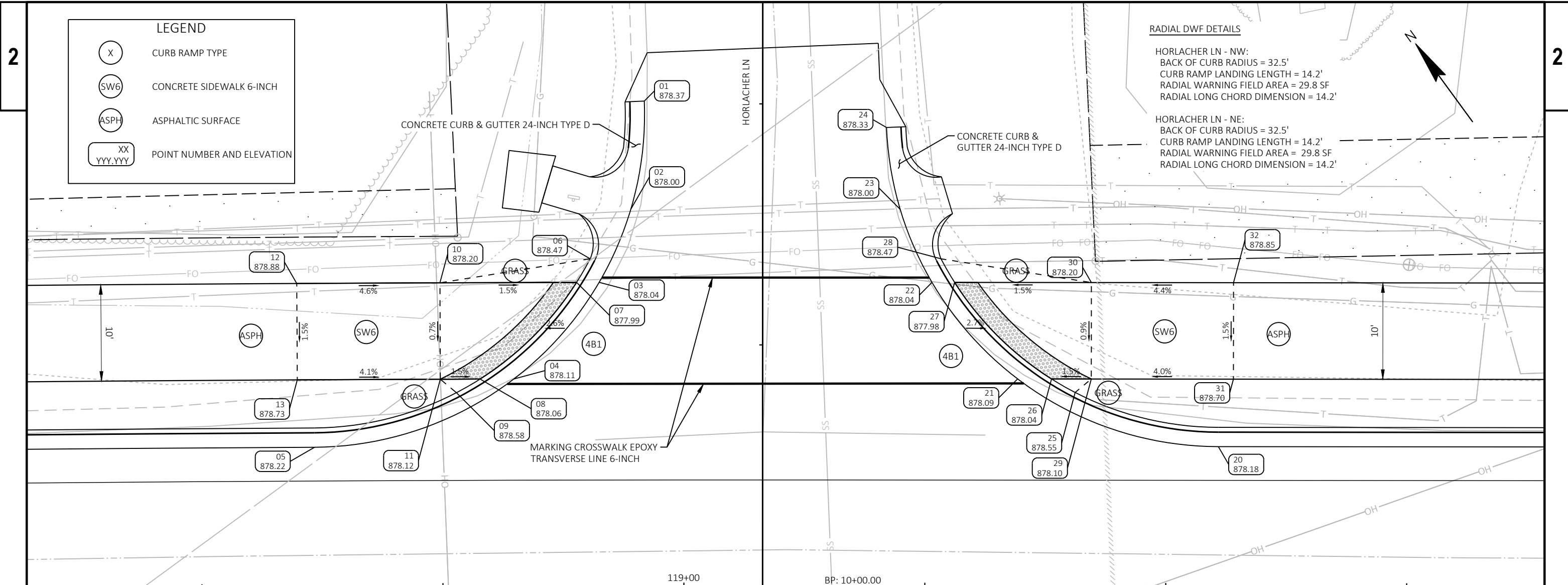


PINE LODGE RD - NW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
01	12+08.84	21.81' LT	879.31	269014.75	352666.01
02	11+90.58	21.77' LT	879.48	268998.91	352657.44
03	12+06.77	24.29' LT	0.00	269014.12	352662.86
04	12+03.73	24.24' LT	0.00	269011.47	352661.49
05	11+95.61	24.22' LT	879.36	269004.43	352657.68
06	11+92.56	24.24' LT	879.89	269001.81	352656.21
07	12+11.80	30.08' LT	0.00	269021.22	352660.10
08	12+03.65	30.20' LT	0.00	269014.23	352656.21
09	11+95.49	30.28' LT	879.75	269007.24	352652.31
10	11+87.33	30.31' LT	880.08	269000.25	352648.41
11	12+12.01	37.77' LT	0.00	269025.03	352653.42
12	12+03.82	37.96' LT	0.00	269018.08	352649.46
13	11+95.34	38.12' LT	879.87	269010.89	352645.36
14	11+87.14	38.23' LT	880.23	269003.93	352641.40

PINE LODGE RD - NE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
20	11+86.32	27.20' RT	0.00	268971.44	352698.21
21	11+96.47	23.32' RT	0.00	268982.36	352699.78
22	12+04.35	22.29' RT	0.00	268989.86	352702.70
23	12+09.26	22.22' RT	0.00	268994.28	352705.00
24	11+87.48	29.40' RT	0.00	268971.41	352700.71
25	11+96.52	25.87' RT	0.00	268981.17	352702.05
26	12+04.38	24.79' RT	0.00	268988.70	352704.92
27	12+07.32	24.75' RT	0.00	268991.35	352706.29
28	12+04.39	25.75' RT	0.00	268988.25	352705.76
29	11+90.27	34.69' RT	0.00	268971.34	352706.71
30	11+96.63	32.06' RT	0.00	268978.30	352707.53
31	12+04.47	31.93' RT	0.00	268985.39	352711.24

NOTES:

- CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES, LENGTHS, AND MATCH POINTS PRIOR TO CURB RAMP AND SIDEWALK CONSTRUCTION
- THE ENGINEER MAY ADJUST ELEVATIONS TO FIT FIELD CONDITIONS
- DASHED LINES SHOWN FOR PROPOSED SIDEWALK ARE FOR INFORMATION ONLY AND DO NOT INDICATE JOINT LOCATIONS
- SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS
- ALL STATION AND OFFSET INFORMATION REFERENCE PINE LODGE RD R/L



LEGEND

- (X) CURB RAMP TYPE
- (SW6) CONCRETE SIDEWALK 6-INCH
- (ASPH) ASPHALTIC SURFACE
- (XX
YYY.YYY) POINT NUMBER AND ELEVATION

RADIAL DWF DETAILS

HORLACHER LN - NW:
 BACK OF CURB RADIUS = 32.5'
 CURB RAMP LANDING LENGTH = 14.2'
 RADIAL WARNING FIELD AREA = 29.8 SF
 RADIAL LONG CHORD DIMENSION = 14.2'

HORLACHER LN - NE:
 BACK OF CURB RADIUS = 32.5'
 CURB RAMP LANDING LENGTH = 14.2'
 RADIAL WARNING FIELD AREA = 29.8 SF
 RADIAL LONG CHORD DIMENSION = 14.2'

HORLACHER LN - NW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
01	118+95.95	50.30' LT	878.37	267927.01	354268.83
02	118+94.55	39.09' LT	878.00	267918.78	354261.09
03	118+91.26	31.50' LT	878.04	267914.58	354253.96
04	118+82.42	21.50' LT	878.11	267911.71	354240.90
05	118+61.75	14.50' LT	878.22	267918.19	354220.02
06	118+90.34	33.95' LT	878.47	267917.11	354254.64
07	118+88.93	31.50' LT	877.99	267915.96	354252.06
08	118+78.92	21.50' LT	878.06	267913.77	354238.06
09	118+76.47	20.10' LT	878.58	267914.08	354235.25
10	118+74.80	31.50' LT	878.20	267924.29	354240.59
11	118+74.80	21.50' LT	878.12	267916.19	354234.72
12	118+60.00	31.50' LT	878.88	267933.00	354228.55
13	118+60.00	21.50' LT	878.73	267924.89	354222.69

HORLACHER LN - NE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
20	119+55.44	14.50' LT	878.18	267862.94	354295.80
21	119+34.61	21.50' LT	878.09	267880.89	354283.12
22	119+25.71	31.50' LT	878.04	267894.22	354281.85
23	119+22.40	39.08' LT	878.00	267902.29	354283.65
24	119+20.97	47.56' LT	878.33	267909.98	354287.51
25	119+40.58	20.10' LT	878.55	267876.24	354287.11
26	119+38.12	21.50' LT	878.04	267878.82	354285.96
27	119+28.06	31.50' LT	877.98	267892.83	354283.74
28	119+26.64	33.95' LT	878.47	267895.64	354284.04
29	119+42.25	21.50' LT	878.10	267876.38	354289.29
30	119+42.25	31.50' LT	878.20	267884.45	354295.19
31	119+57.00	21.50' LT	878.70	267867.67	354301.19
32	119+57.00	31.50' LT	878.85	267875.74	354307.09

NOTES:

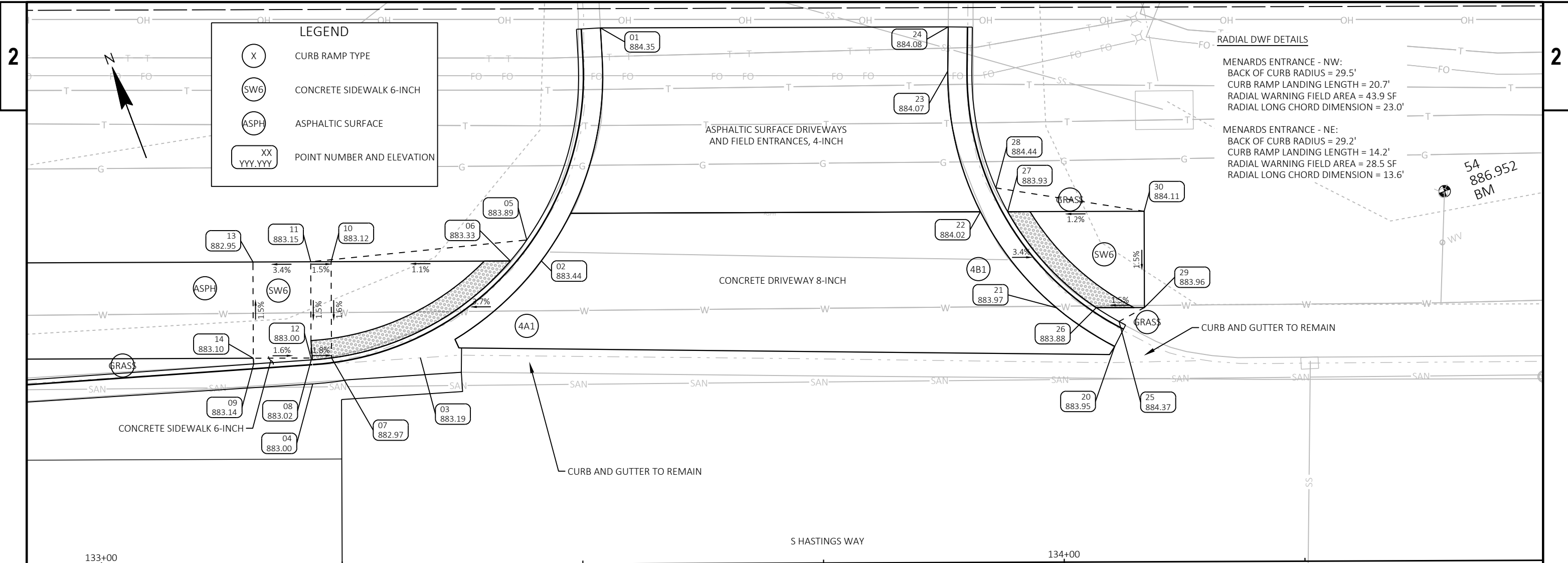
CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES, LENGTHS, AND MATCH POINTS PRIOR TO CURB RAMP AND SIDEWALK CONSTRUCTION

THE ENGINEER MAY ADJUST ELEVATIONS TO FIT FIELD CONDITIONS

DASHED LINES SHOWN FOR PROPOSED SIDEWALK ARE FOR INFORMATION ONLY AND DO NOT INDICATE JOINT LOCATIONS

SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS

ALL STATION AND OFFSET INFORMATION REFERENCE S HASTINGS WAY R/L



LEGEND

- (X) CURB RAMP TYPE
- (SW6) CONCRETE SIDEWALK 6-INCH
- (ASPH) ASPHALTIC SURFACE
- (XX
YYY.YYY) POINT NUMBER AND ELEVATION

RADIAL DWF DETAILS

MENARDS ENTRANCE - NW:
 BACK OF CURB RADIUS = 29.5'
 CURB RAMP LANDING LENGTH = 20.7'
 RADIAL WARNING FIELD AREA = 43.9 SF
 RADIAL LONG CHORD DIMENSION = 23.0'

MENARDS ENTRANCE - NE:
 BACK OF CURB RADIUS = 29.2'
 CURB RAMP LANDING LENGTH = 14.2'
 RADIAL WARNING FIELD AREA = 28.5 SF
 RADIAL LONG CHORD DIMENSION = 13.6'

MENARDS ENTRANCE - NW					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
01	133+52.04	55.68' LT	884.35	267253.37	355530.20
02	133+45.80	31.50' LT	883.44	267232.89	355515.91
03	133+33.19	21.50' LT	883.19	267227.91	355500.61
04	133+22.01	18.81' LT	883.00	267229.30	355489.19
05	133+44.30	33.68' LT	883.89	267235.45	355515.26
06	133+42.59	31.50' LT	883.33	267234.01	355512.90
07	133+23.97	21.50' LT	882.97	267231.13	355491.97
08	133+21.86	21.31' LT	883.02	267231.69	355489.92
09	133+15.89	20.91' LT	883.14	267233.40	355484.19
10	133+23.97	31.50' LT	883.12	267240.50	355495.45
11	133+21.85	31.50' LT	883.15	267241.24	355493.47
12	133+21.85	21.50' LT	883.00	267231.87	355489.98
13	133+15.85	31.50' LT	882.95	267243.34	355487.84
14	133+15.85	21.50' LT	883.10	267233.97	355484.36

MENARDS ENTRANCE - NE					
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
20	134+05.32	22.39' LT	883.95	267203.58	355568.51
21	133+99.28	26.42' LT	883.97	267209.46	355564.26
22	133+91.43	36.43' LT	884.02	267221.58	355560.39
23	133+88.09	51.05' LT	884.07	267236.45	355562.37
24	133+88.16	55.65' LT	884.08	267240.73	355564.03
25	134+05.78	24.98' LT	884.37	267205.84	355569.84
26	134+03.42	26.42' LT	883.88	267208.02	355568.14
27	133+94.26	36.43' LT	883.93	267220.60	355563.04
28	133+93.03	38.91' LT	884.44	267223.34	355562.76
29	134+08.42	26.41' LT	883.96	267206.27	355572.82
30	134+08.43	36.41' LT	884.11	267215.63	355576.32

NOTES:

CONTRACTOR TO FIELD VERIFY ELEVATION, GRADES, SLOPES, LENGTHS, AND MATCH POINTS PRIOR TO CURB RAMP AND SIDEWALK CONSTRUCTION

THE ENGINEER MAY ADJUST ELEVATIONS TO FIT FIELD CONDITIONS

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SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS

ALL STATION AND OFFSET INFORMATION REFERENCE S HASTINGS WAY R/L

Estimate Of Quantities

7995-02-65

Line	Item	Item Description	Unit	Total	Qty
0002	201.0205	Grubbing	STA	9.000	9.000
0004	203.0100	Removing Small Pipe Culverts	EACH	1.000	1.000
0006	204.0100	Removing Concrete Pavement	SY	64.000	64.000
0008	204.0110	Removing Asphaltic Surface	SY	121.000	121.000
0010	204.0150	Removing Curb & Gutter	LF	425.000	425.000
0012	204.0155	Removing Concrete Sidewalk	SY	21.000	21.000
0014	204.0190	Removing Surface Drains	EACH	1.000	1.000
0016	204.0210	Removing Manholes	EACH	2.000	2.000
0018	204.0215	Removing Catch Basins	EACH	1.000	1.000
0020	204.0220	Removing Inlets	EACH	2.000	2.000
0022	204.0245	Removing Storm Sewer (size) 01. 12-Inch	LF	165.000	165.000
0024	204.0245	Removing Storm Sewer (size) 02. 21-Inch	LF	16.000	16.000
0026	204.0245	Removing Storm Sewer (size) 03. 24-Inch	LF	32.000	32.000
0028	204.0245	Removing Storm Sewer (size) 04. 48-Inch	LF	65.000	65.000
0030	204.9060.S	Removing (item description) 01. Concrete Pedestal	EACH	2.000	2.000
0032	205.0100	Excavation Common	CY	11,572.000	11,572.000
0034	213.0100	Finishing Roadway (project) 01. 7995-02-65	EACH	1.000	1.000
0036	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	7,070.000	7,070.000
0038	416.0160	Concrete Driveway 6-Inch	SY	151.000	151.000
0040	416.0180	Concrete Driveway 8-Inch	SY	87.000	87.000
0042	416.1010	Concrete Surface Drains	CY	2.000	2.000
0044	455.0605	Tack Coat	GAL	751.000	751.000
0046	460.2000	Incentive Density HMA Pavement	DOL	2,320.000	2,320.000
0048	460.5223	HMA Pavement 3 LT 58-28 S	TON	2,410.000	2,410.000
0050	460.5244	HMA Pavement 4 LT 58-34 S	TON	1,210.000	1,210.000
0052	465.0105	Asphaltic Surface	TON	710.000	710.000
0054	465.0110	Asphaltic Surface Patching	TON	6.000	6.000
0056	465.0120	Asphaltic Surface Driveways and Field Entrances	TON	20.000	20.000
0058	520.8000	Concrete Collars for Pipe	EACH	8.000	8.000
0060	522.1030	Apron Endwalls for Culvert Pipe Reinforced Concrete 30-Inch	EACH	2.000	2.000
0062	601.0411	Concrete Curb & Gutter 30-Inch Type D	LF	139.000	139.000
0064	602.0405	Concrete Sidewalk 4-Inch	SF	1,510.000	1,510.000
0066	602.0415	Concrete Sidewalk 6-Inch	SF	1,147.000	1,147.000
0068	602.0515	Curb Ramp Detectable Warning Field Natural Patina	SF	32.000	32.000
0070	602.0615	Curb Ramp Detectable Warning Field Radial Natural Patina	SF	133.000	133.000
0072	606.0100	Riprap Light	CY	1.000	1.000
0074	608.0312	Storm Sewer Pipe Reinforced Concrete Class III 12-Inch	LF	301.000	301.000
0076	608.0315	Storm Sewer Pipe Reinforced Concrete Class III 15-Inch	LF	52.000	52.000
0078	608.0318	Storm Sewer Pipe Reinforced Concrete Class III 18-Inch	LF	8.000	8.000
0080	608.0321	Storm Sewer Pipe Reinforced Concrete Class III 21-Inch	LF	16.000	16.000
0082	608.0324	Storm Sewer Pipe Reinforced Concrete Class III 24-Inch	LF	32.000	32.000
0084	608.0330	Storm Sewer Pipe Reinforced Concrete Class III 30-Inch	LF	38.000	38.000
0086	608.0348	Storm Sewer Pipe Reinforced Concrete Class III 48-Inch	LF	63.000	63.000
0088	608.0412	Storm Sewer Pipe Reinforced Concrete Class IV 12-Inch	LF	395.000	395.000
0090	608.0415	Storm Sewer Pipe Reinforced Concrete Class IV 15-Inch	LF	276.000	276.000
0092	608.0421	Storm Sewer Pipe Reinforced Concrete Class IV 21-Inch	LF	148.000	148.000
0094	608.0430	Storm Sewer Pipe Reinforced Concrete Class IV 30-Inch	LF	31.000	31.000
0096	611.0420	Reconstructing Manholes	EACH	3.000	3.000
0098	611.0530	Manhole Covers Type J	EACH	4.000	4.000

Estimate Of Quantities

7995-02-65

Line	Item	Item Description	Unit	Total	Qty
0100	611.0624	Inlet Covers Type H	EACH	26.000	26.000
0102	611.1005	Catch Basins 5-FT Diameter	EACH	1.000	1.000
0104	611.1006	Catch Basins 6-FT Diameter	EACH	1.000	1.000
0106	611.2003	Manholes 3-FT Diameter	EACH	1.000	1.000
0108	611.2004	Manholes 4-FT Diameter	EACH	2.000	2.000
0110	611.2005	Manholes 5-FT Diameter	EACH	1.000	1.000
0112	611.2007	Manholes 7-FT Diameter	EACH	2.000	2.000
0114	611.3004	Inlets 4-FT Diameter	EACH	1.000	1.000
0116	611.3230	Inlets 2x3-FT	EACH	20.000	20.000
0118	611.8110	Adjusting Manhole Covers	EACH	4.000	4.000
0120	616.0700.S	Fence Safety	LF	311.000	311.000
0122	618.0100	Maintenance And Repair of Haul Roads (project) 01. 7995-02-65	EACH	1.000	1.000
0124	619.1000	Mobilization	EACH	1.000	1.000
0126	624.0100	Water	MGAL	143.000	143.000
0128	625.0100	Topsoil	SY	1,580.000	1,580.000
0130	625.0500	Salvaged Topsoil	SY	6,780.000	6,780.000
0132	627.0200	Mulching	SY	6,780.000	6,780.000
0134	628.1504	Silt Fence	LF	1,105.000	1,105.000
0136	628.1520	Silt Fence Maintenance	LF	1,105.000	1,105.000
0138	628.1905	Mobilizations Erosion Control	EACH	3.000	3.000
0140	628.1910	Mobilizations Emergency Erosion Control	EACH	3.000	3.000
0142	628.2008	Erosion Mat Urban Class I Type B	SY	1,580.000	1,580.000
0144	628.6005	Turbidity Barriers	SY	794.000	794.000
0146	628.7005	Inlet Protection Type A	EACH	24.000	24.000
0148	628.7015	Inlet Protection Type C	EACH	21.000	21.000
0150	629.0210	Fertilizer Type B	CWT	5.700	5.700
0152	630.0110	Seeding Mixture No. 10	LB	117.000	117.000
0154	630.0200	Seeding Temporary	LB	233.000	233.000
0156	638.2102	Moving Signs Type II	EACH	11.000	11.000
0158	638.4000	Moving Small Sign Supports	EACH	11.000	11.000
0160	642.5001	Field Office Type B	EACH	1.000	1.000
0162	643.0300	Traffic Control Drums	DAY	2,535.000	2,535.000
0164	643.0420	Traffic Control Barricades Type III	DAY	2,145.000	2,145.000
0166	643.0705	Traffic Control Warning Lights Type A	DAY	4,290.000	4,290.000
0168	643.0900	Traffic Control Signs	DAY	1,755.000	1,755.000
0170	643.1050	Traffic Control Signs PCMS	DAY	14.000	14.000
0172	643.5000	Traffic Control	EACH	1.000	1.000
0174	645.0130	Geotextile Type R	SY	10.000	10.000
0176	646.1020	Marking Line Epoxy 4-Inch	LF	12,920.000	12,920.000
0178	646.7420	Marking Crosswalk Epoxy Transverse Line 6-Inch	LF	90.000	90.000
0180	646.7520	Marking Crosswalk Epoxy Block Style 24-Inch	LF	88.000	88.000
0182	650.4000	Construction Staking Storm Sewer	EACH	62.000	62.000
0184	650.4500	Construction Staking Subgrade	LF	3,370.000	3,370.000
0186	650.5000	Construction Staking Base	LF	3,370.000	3,370.000
0188	650.5500	Construction Staking Curb Gutter and Curb & Gutter	LF	6,610.000	6,610.000
0190	650.9000	Construction Staking Curb Ramps	EACH	6.000	6.000
0192	650.9500	Construction Staking Sidewalk (project) 01. 7995-02-65	EACH	1.000	1.000
0194	650.9911	Construction Staking Supplemental Control (project) 01. 7995-02-65	EACH	1.000	1.000
0196	650.9920	Construction Staking Slope Stakes	LF	3,370.000	3,370.000

Estimate Of Quantities

7995-02-65

Line	Item	Item Description	Unit	Total	Qty
0198	690.0150	Sawing Asphalt	LF	202.000	202.000
0200	690.0250	Sawing Concrete	LF	109.000	109.000
0202	ASP.1T0A	On-the-Job Training Apprentice at \$5.00/HR	HRS	300.000	300.000
0204	ASP.1T0G	On-the-Job Training Graduate at \$5.00/HR	HRS	300.000	300.000
0206	SPV.0060	Special 01. Remove & Relocate Hydrant	EACH	1.000	1.000
0208	SPV.0060	Special 02. Supply & Install Hydrant	EACH	1.000	1.000
0210	SPV.0060	Special 03. 6-Inch Ductile Iron Hydrant Lead	EACH	56.000	56.000
0212	SPV.0060	Special 04. 6-Inch Valve & Box	EACH	1.000	1.000
0214	SPV.0060	Special 05. Adjust Valve & Box	EACH	2.000	2.000
0216	SPV.0060	Special 07. Connect to Existing Storm Sewer Pipe	EACH	10.000	10.000
0218	SPV.0060	Special 08. Connect To Existing Storm Sewer Manhole	EACH	4.000	4.000
0220	SPV.0060	Special 09. Remove Hydrant	EACH	1.000	1.000
0222	SPV.0090	Special 01. Concrete Curb & Gutter 24-inch Type D	LF	6,610.000	6,610.000

3

3

CATEGORY	STATION	TO	STATION	LOCATION	201.0205 GRUBBING STA	CATEGORY	STATION	LOCATION	203.0100 REMOVING SMALL PIPE CULVERTS EACH	REMARKS	
0010	100+40	-	102+00	LT	2						
0010	122+00	-	124+00	LT	2						
0010	128+00	-	133+00	LT	5						
TOTAL 0010					9						
0010	121+77			ML		0010	121+77	ML	1	30-INCH	
TOTAL 0010						TOTAL 0010					1

CATEGORY	STATION	TO	STATION	LOCATION	204.0100 REMOVING CONCRETE PAVEMENT SY	204.0110 REMOVING ASPHALTIC SURFACE SY	204.0150 REMOVING CURB & GUTTER LF	204.0155 REMOVING CONCRETE SIDEWALK SY
0010	11+91	-	12+11	LT	-	5	18	21
0010	11+92	-	12+11	RT	-	6	23	-
0010	101+00	-	101+77	LT	-	-	78	-
0010	132+25	-	134+06	LT	64	110	200	-
0010	132+19	-	133+25	RT	-	-	106	-
TOTAL 0010					64	121	425	21

NOTE: ASPHALT REMOVAL BETWEEN STATION 101+00 - STATION 133+25 SHALL BE PAID UNDER THE EXCAVATION COMMON ITEM.

CATEGORY	STATION	TO	STATION	LOCATION	204.0245.01 REMOVING STORM SEWER (SIZE) (01. 12- INCH) LF	204.0245.02 REMOVING STORM SEWER (SIZE) (02. 21- INCH) LF	204.0245.03 REMOVING STORM SEWER (SIZE) (03. 24- INCH) LF	204.0245.04 REMOVING STORM SEWER (SIZE) (04. 48- INCH) LF
0010	101+39	-	101+52	LT/RT	50	--	--	--
0010	101+52	-	102+11	LT	67	--	--	--
0010	101+58	-	101+74	RT	--	--	16	--
0010	108+82	-	108+98	RT	--	--	16	--
0010	111+42	-	111+58	RT	--	16	--	--
0010	118+75	-	119+16	RT	48	--	--	--
0010	131+99	-	132+27	LT/RT	--	--	--	65
TOTAL 0010					165	16	32	65

CATEGORY	STATION	LOCATION	204.0210 REMOVING MANHOLES EACH	204.0215 REMOVING CATCH BASINS EACH	204.0220 REMOVING INLETS EACH
0010	101+50	24'LT	--	--	1
0010	102+10	27'LT	--	1	--
0010	119+16	16'RT	1	--	--
0010	132+04	11'LT	1	--	--
0010	132+24	19'RT	--	--	1
TOTAL 0010			2	1	2

CATEGORY	STATION	LOCATION	204.9060.S.01 REMOVING (ITEM DESCRIPTION) (01. CONCRETE PEDESTAL) EACH
0010	103+00	LT	1
0010	103+18	LT	1
TOTAL 0010			2

CATEGORY	STATION	LOCATION	204.0190 REMOVING SURFACE DRAINS EACH	
0010	101+05	RT	1	
TOTAL 0010				1

DIVISION	FROM/TO STATION	LOCATION	205.0100 COMMON EXCAVATION (1)	SALVAGED/UNUSABLE PAVEMENT MATERIAL (3)	AVAILABLE MATERIAL (4)	UNEXPANDED FILL	EXPANDED FILL (5)	MASS ORDINATE +/- (6)	WASTE	
			CUT (2)				FACTOR 1.20			
DIVISION 1										
QUANTITY REPORT TEMP XML-220210			100+25/133+00	11,572	1,220	10,352	2,074	2,489	7,863	7,863
DIVISION 1 SUBTOTAL				11,572	1,220	10,352	2,074	2,489	7,863	7,863
GRAND TOTAL				11,572	1,220	10,352	2,074	2,489	7,863	7,863
TOTAL COMMON EXC				11,572						

NOTES:
 (1) COMMON EXCAVATION IS THE SUM OF THE CUT AND FBS EXCAVATION COLUMNS. ITEM NUMBER 205.0100
 (2) SALVAGED/UNUSABLE PAVEMENT MATERIAL IS INCLUDED IN CUT.
 (3) SALVAGED/UNUSABLE PAVEMENT MATERIAL
 (4) AVAILABLE MATERIAL = CUT - SALVAGED/UNUSABLE PAVEMENT MATERIAL
 (5) EXPANDED FILL FACTOR = 1.20
 (6) THE MASS ORDINATE + OR - QTY CALCULATED FOR THE DIVISION. PLUS QUANTITY INDICATES AN EXCESS OF MATERIAL WITHIN THE DIVISION. MINUS INDICATES A SHORTAGE OF MATERIAL WITHIN THE DIVISION.
 (7) FACTORS USED TO COMPUTE ANTICIPATED WASTE AND THE COMPUTED WASTE VOLUME IDENTIFIED ARE FOR GENERAL INFORMATION ONLY.

3

CATEGORY	STATION	TO	STATION	LOCATION	305.0120 BASE AGGREGATE DENSE 1 1/4- INCH TON	624.0100 WATER MGAL
0010	100+05	-	133+25	MULTI USE PATH	1,350	28
0010	100+10	-	133+25	MAINLINE	5,720	115
TOTAL 0010					7,070	143

CATEGORY	STATION	TO	STATION	LOCATION	416.0160 CONCRETE DRIVEWAY 6- INCH SY	416.0180 CONCRETE DRIVEWAY 8- INCH SY
0010	102+31	-	102+75	LT	48	-
0010	102+95	-	103+18	LT	22	-
0010	106+25	-	106+48	LT	29	-
0010	110+61	-	110+98	LT	52	-
0010	133+37	-	134+05	LT	-	87
TOTAL 0010					151	87

CATEGORY	STATION	LOCATION	416.1010 CONCRETE SURFACE DRAINS CY	606.0100 RIPRAP LIGHT CY	645.0130 GEOTEXTILE TYPER SY
0010	118+85	LT	2	1	10
TOTAL 0010			2	1	10

3

CATEGORY	STATION	TO	STATION	LOCATION	455.0605 TACK COAT GAL	460.5223 HMA PAVEMENT 3 LT 58-28 S TON	460.5244 HMA PAVEMENT 4 LT 58-34 S TON	465.0105 ASPHALTIC SURFACE TON	465.0120 ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES TON
0010	100+05	-	133+25	MAINLINE	751	2,410	1,210	--	20
0010	103+15	-	133+25	MULTI USE PATH	--	--	--	710	--
TOTAL 0010					751	2,410	1,210	710	20

CATEGORY	STATION	TO	STATION	LOCATION	465.0110 ASPHALTIC SURFACE PATCHING TON	602.0415 CONCRETE SIDEWALK 6- INCH SF	602.0515 CURB RAMP DETECTABLE WARNING FIELD NATURAL PATINA SF	602.0615 CURB RAMP DETECTABLE WARNING FIELD RADIAL NATURAL PATINA SF
0010	11+87	-	12+14	LT	2	237	16	-
0010	11+91	-	12+11	RT	2	106	16	-
0010	118+60	-	118+89	LT	-	248	-	30
0010	119+28	-	119+57	LT	-	247	-	30
0010	133+16	-	133+43	LT	2	205	-	44
0010	133+94	-	134+08	LT	-	104	-	29
TOTAL 0010					6	1,147	32	133

CATEGORY	STATION	TO	STATION	LOCATION	602.0405 CONCRETE SIDEWALK 4- INCH SF
0010	100+15	-	103+15	LT	1,510
TOTAL 0010					1,510

CATEGORY	STATION	TO	STATION	LOCATION	616.0700.S FENCE SAFETY LF
0010	103+10	-	106+20	LT	311
TOTAL 0010					311

CATEGORY	STATION	LOCATION	570.8000 CONCRETE COLLARS FOR PIPE EACH	REMARKS
0010	107+06	RT	1	24" CONCRETE PIPE JOINT
0010	107+15	RI	1	24" CONCRETE PIPE JOINT
0010	107+62	RT	1	24" CONCRETE PIPE JOINT
0010	108+52	RI	1	24" CONCRETE PIPE JOINT
0010	109+98	RT	1	24" CONCRETE PIPE JOINT
0010	110+68	RT	1	21" CONCRETE PIPE JOINT
0010	110+93	RT	1	21" CONCRETE PIPE JOINT
0010	114+23	RI	1	18" CONCRETE PIPE JOINT
TOTAL 0010			8	

CATEGORY	STATION	LOCATION	611.0420 RECONSTRUCTING MANHOLES EACH	* 611.8110 ADJUSTING MANHOLE COVERS EACH
0010	101+39	23' RT	1	--
0010	104+38	23' RT	--	1
0010	110+36	23' RT	--	1
0010	113+35	22' RT	1	--
0010	116+33	20' RT	1	--
TOTAL 0010			3	2

CATEGORY	STATION	LOCATION	628.7005 INLET PROTECTION TYPE A EACH	628.7015 INLET PROTECTION TYPE C EACH
0010	101+37	33' RT	1	--
0010	101+63	49' LT	1	--
0010	132+33	40' RT	1	--
0010	--	ML INLETS	21	21
TOTAL 0010			24	21

* QUANTITIES SHOWN ELSEHERE IN THE PLAN

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CATEGORY	FROM STRUCT.	TO STRUCT.	INLET ELEV.	DISCHARGE ELEV.	SLOPE	608.0312	608.0315	608.0318	608.0321	608.0324	608.0330	608.0348	608.0412	608.0415	608.0421	608.0430	SPV.0060.07	SPV.0060.08
						STORM SEWER PIPE REINFORCED CONCRETE CLASS III 12-INCH LF	STORM SEWER PIPE REINFORCED CONCRETE CLASS III 15-INCH LF	STORM SEWER PIPE REINFORCED CONCRETE CLASS III 18-INCH LF	STORM SEWER PIPE REINFORCED CONCRETE CLASS III 21-INCH LF	STORM SEWER PIPE REINFORCED CONCRETE CLASS III 24-INCH LF	STORM SEWER PIPE REINFORCED CONCRETE CLASS III 30-INCH LF	STORM SEWER PIPE REINFORCED CONCRETE CLASS III 48-INCH LF	STORM SEWER PIPE REINFORCED CONCRETE CLASS IV 12-INCH LF	STORM SEWER PIPE REINFORCED CONCRETE CLASS IV 15-INCH LF	STORM SEWER PIPE REINFORCED CONCRETE CLASS IV 21-INCH LF	STORM SEWER PIPE REINFORCED CONCRETE CLASS IV 30-INCH LF	SPECIAL (07. CONNECT TO EXISTING STORM SEWER PIPE) EACH	SPECIAL (08. CONNECT TO EXISTING STORM SEWER MANHOLE) EACH
0010	EXISTING SS PIPE	1A	867.82	867.80	0.20%	--	--	--	--	8	--	--	--	--	--	--	1	--
0010	1A	EXISTING SS PIPE	867.80	867.78	0.20%	--	--	--	--	8	--	--	--	--	--	--	1	--
0010	1B	1A	872.80	872.72	1.00%	--	--	8	--	--	--	--	--	--	--	--	--	--
0010	1C	1B	873.40	873.30	1.00%	10	--	--	--	--	--	--	--	--	--	--	--	--
0010	1D	1B	873.45	873.05	1.00%	--	40	--	--	--	--	--	--	--	--	--	--	--
0010	1E	1D	873.80	873.70	1.00%	10	--	--	--	--	--	--	--	--	--	--	--	--
0010	1F	EXISTING SS MANHOLE	874.84	874.77	1.00%	7	--	--	--	--	--	--	--	--	--	--	--	1
0010	1G	1F	875.25	874.94	1.00%	31	--	--	--	--	--	--	--	--	--	--	--	--
0010	2A	EXISTING SS MANHOLE	875.60	875.22	1.00%	38	--	--	--	--	--	--	--	--	--	--	--	1
0010	EXISTING SS PIPE	2B	869.25	869.24	0.16%	--	--	--	--	8	--	--	--	--	--	--	1	--
0010	2B	EXISTING SS PIPE	869.24	869.23	0.16%	--	--	--	--	8	--	--	--	--	--	--	1	--
0010	2C	2B	874.14	874.07	1.00%	--	7	--	--	--	--	--	--	--	--	--	--	--
0010	2D	2C	874.70	874.39	1.00%	31	--	--	--	--	--	--	--	--	--	--	--	--
0010	2E	2D	874.90	874.80	1.00%	10	--	--	--	--	--	--	--	--	--	--	--	--
0010	EXISTING SS PIPE	2F	869.64	869.63	0.12%	--	--	--	8	--	--	--	--	--	--	--	1	--
0010	2F	EXISTING SS PIPE	869.63	869.62	0.12%	--	--	--	8	--	--	--	--	--	--	--	1	--
0010	2G	2F	874.70	874.32	1.00%	38	--	--	--	--	--	--	--	--	--	--	--	--
0010	3A	EXISTING SS MANHOLE	873.24	873.18	1.00%	6	--	--	--	--	--	--	--	--	--	--	--	1
0010	3B	3A	873.65	873.34	1.00%	31	--	--	--	--	--	--	--	--	--	--	--	--
0010	3C	EXISTING SS MANHOLE	871.34	871.29	1.00%	--	5	--	--	--	--	--	--	--	--	--	--	1
0010	3D	3C	871.90	871.59	1.00%	31	--	--	--	--	--	--	--	--	--	--	--	--
0010	3E	3D	872.10	872.00	1.00%	10	--	--	--	--	--	--	--	--	--	--	--	--
0010	EXISTING SS PIPE	4A	871.11	871.09	0.20%	8	--	--	--	--	--	--	--	--	--	--	1	--
0010	4A	EXISTING SS PIPE	871.05	870.98	0.18%	40	--	--	--	--	--	--	--	--	--	--	1	--
0010	4B	4C	872.60	872.30	4.92%	--	--	--	--	--	6	--	--	--	--	--	--	--
0010	4C	4D	872.29	872.20	0.30%	--	--	--	--	--	--	--	--	--	--	31	--	--
0010	4D	4E	872.19	872.10	0.30%	--	--	--	--	--	32	--	--	--	--	--	--	--
0010	4F	4C	872.67	872.30	0.25%	--	--	--	--	--	--	--	--	148	--	--	--	--
0010	4G	4F	873.25	873.10	0.50%	--	--	--	--	--	--	31	--	--	--	--	--	--
0010	5A	4F	873.85	872.74	0.40%	--	--	--	--	--	--	--	276	--	--	--	--	--
0010	5B	5A	874.25	874.10	0.50%	--	--	--	--	--	--	31	--	--	--	--	--	--
0010	5C	5A	875.61	874.10	0.50%	--	--	--	--	--	--	302	--	--	--	--	--	--
0010	5D	5C	875.86	875.71	0.50%	--	--	--	--	--	--	31	--	--	--	--	--	--
0010	EXISTING SS PIPE	6A	872.25	872.20	0.31%	--	--	--	--	--	--	16	--	--	--	--	1	--
0010	6A	6B	872.20	871.45	2.07%	--	--	--	--	--	36	--	--	--	--	--	--	--
0010	6B	EXISTING SS PIPE	871.45	871.40	0.44%	--	--	--	--	--	11	--	--	--	--	--	1	--
TOTAL 0010						301	52	8	16	32	38	63	395	276	148	31	10	4

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CATEGORY	STRUCT. NUMBER	STATION	OFFSET	CASTING ELEV.	522.1030	611.0530	611.0624	611.1005	611.1006	611.2003	611.2004	611.2005	611.2007	611.3004	611.3230
					APRON ENDWALLS FOR CULVERT PIPE REINFORCED CONCRETE 30-INCH EACH	MANHOLE COVERS TYPE J EACH	INLET COVERS TYPE H EACH	CATCH BASINS 5- FT DIAMETER EACH	CATCH BASINS 6- FT DIAMETER EACH	MANHOLES 3-FT DIAMETER EACH	MANHOLES 4-FT DIAMETER EACH	MANHOLES 5-FT DIAMETER EACH	MANHOLES 7-FT DIAMETER EACH	INLETS 4-FT DIAMETER EACH	INLETS 2X3-FT EACH
0010	1A	101+66	23.4' RT	877.28	--	1	--	--	--	--	--	1	--	--	--
0010	1B	101+66	15.5' RT	878.09	--	--	1	--	--	--	--	--	--	--	1
0010	1C	101+76	15.5' RT	878.10	--	--	1	--	--	--	--	--	--	--	1
0010	1D	101+66	24.5' LT	877.76	--	--	1	--	--	--	--	--	--	--	1
0010	1E	101+76	24.5' LT	877.78	--	--	1	--	--	--	--	--	--	--	1
0010	1F	104+38	15.5' RT	879.62	--	--	1	--	--	--	--	--	--	--	1
0010	1G	104+38	15.5' LT	879.62	--	--	1	--	--	--	--	--	--	--	1
0010	2A	107+30	15.5' LT	879.94	--	--	1	--	--	--	--	--	--	--	1
0010	2B	108+90	22.5' RT	879.26	--	1	--	--	--	--	1	--	--	--	--
0010	2C	108+90	15.5' RT	879.27	--	--	1	--	--	--	--	--	--	--	1
0010	2D	108+90	15.5' LT	879.27	--	--	1	--	--	--	--	--	--	--	1
0010	2E	109+00	15.5' LT	879.27	--	--	1	--	--	--	--	--	--	--	1
0010	2F	111+50	22.4' RT	878.55	--	1	--	--	--	--	1	--	--	--	--
0010	2G	111+50	15.5' LT	879.12	--	--	1	--	--	--	--	--	--	--	1
0010	3A	113+34	15.5' RT	878.08	--	--	1	--	--	--	--	--	--	--	1
0010	3B	113+34	15.5' LT	878.08	--	--	1	--	--	--	--	--	--	--	1
0010	3C	116+33	15.5' RT	876.55	--	--	1	--	--	--	--	--	--	--	1
0010	3D	116+33	15.5' LT	876.55	--	--	1	--	--	--	--	--	--	--	1
0010	3E	116+23	15.5' LT	876.55	--	--	1	--	--	--	--	--	--	--	--
0010	4A	119+15	19.4' RT	877.46	--	1	--	--	--	1	--	--	--	--	--
0010	4B	121+77	21.6' RT	-	1	--	--	--	--	--	--	--	--	--	--
0010	4C	121+77	15.5' RT	876.42	--	--	1	--	1	--	--	--	--	--	--
0010	4D	121+77	15.5' LT	876.42	--	--	1	1	--	--	--	--	--	--	--
0010	4E	121+77	47.0' LT	-	1	--	--	--	--	--	--	--	--	--	--
0010	4F	123+25	15.5' RT	875.89	--	--	1	--	--	--	--	--	--	1	--
0010	4G	123+25	15.5' LT	875.89	--	--	1	--	--	--	--	--	--	--	1
0010	5A	126+00	15.5' RT	877.03	--	--	1	--	--	--	--	--	--	--	1
0010	5B	126+00	15.5' LT	877.03	--	--	1	--	--	--	--	--	--	--	1
0010	5C	129+00	15.5' RT	878.60	--	--	1	--	--	--	--	--	--	--	1
0010	5D	129+00	15.5' LT	878.60	--	--	1	--	--	--	--	--	--	--	1
0010	6A	132+20	15.5' RT	881.88	--	--	1	--	--	--	--	--	1	--	--
0010	6B	132+01	15.5' LT	881.67	--	--	1	--	--	--	--	--	1	--	--
TOTAL 0010					2	4	26	1	1	1	2	1	2	1	20

CATEGORY	STATION	TO	STATION	LOCATION	625.0100	625.0500	627.0200	628.2004	629.0210	630.0110	630.0200
					TOPSOIL SY	SALVAGED TOPSOIL SY	MULCHING SY	EROSION MAT CLASS I TYPE B SY	FERTILIZER TYPE B CWT	SEEDING MIXTURE NO. 10 LB	SEEDING TEMPORARY LB
0010	11+87	-	12+14	LT	20	--	--	20	0.1	1	1
0010	100+00	-	118+90	TERRACE	930	--	--	930	0.6	13	26
0010	119+28	-	133+25	TERRACE	310	--	--	310	0.2	5	10
0010	100+05	-	118+90	LT	--	800	800	--	0.6	11	22
0010	119+28	-	133+25	LT	--	2,360	2,360	--	1.5	32	64
0010	101+00	-	133+25	RT	--	2,260	2,260	--	1.5	31	62
0010				UNDISTRIBUTED	320	1,360	1,360	320	1.2	24	48
TOTAL 0010					1,580	6,780	6,780	1,580	5.7	117	233

PROJECT NO: 7995-02-65

HWY: S HASTINGS WAY

COUNTY: EAU CLAIRE

MISCELLANEOUS QUANTITIES

SHEET

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CATEGORY	STATION	TO	STATION	LOCATION	628.1504 SILT FENCE LF	628.1520 SILT FENCE MAINTENANCE LF	628.6005 TURBIDITY BARRIERS SY	REMARKS
0010	101+00	-	107+50	RT	658	658	--	
0010	119+30	-	133+50	LT	--	--	635	SILT FENCE SHOULD BE USED IF WATER LEVELS ALLOW
0010	131+00	-	133+25	RT	226	226	--	
0010				UNDISTRIBUTED	221	221	159	
				TOTAL 0010	1,105	1,105	794	

CATEGORY	STATION	LOCATION	638.2102 MOVING SIGNS TYPE II EACH	638.4000 MOVING SMALL SIGN SUPPORTS EACH	REMARKS
0010	100+44	RT	--	--	STOP
0010	100+44	LT	1	1	STOP
0010	102+20	LT	1	1	CROSS AHEAD
0010	103+48	RT	1	1	SPEED LIMIT 40
0010	118+20	LT	1	1	SPEED LIMIT 40
0010	118+90	LT	1	1	STOP
0010	120+42	RT	1	1	SPEED LIMIT 40
0010	126+78	LT	1	1	SPEED LIMIT 40
0010	131+44	LT	1	1	SPEED LIMIT 40
0010	131+67	RT	1	1	CHEVRON
0010	132+27	LT	1	1	CHEVRON
0010	133+04	LT	1	1	CHEVRON
		TOTAL 0010	11	11	

CATEGORY	LOCATION	643.0300 TRAFFIC CONTROL DRUMS DAY	643.0420 TRAFFIC CONTROL BARRICADES TYPE II DAY	643.0705 TRAFFIC CONTROL WARNING LIGHTS TYPE A DAY	643.0900 TRAFFIC CONTROL SIGNS DAY	643.1050 TRAFFIC CONTROL SIGNS PCMS DAY
0010	BEGIN PROJECT	2,028	624	1,248	702	7
0010	HORLACHER DR	--	546	1,092	234	--
0010	END PROJECT	--	546	1,092	468	7
0010	UNDISTRIBUTED (25%)	507	429	858	351	--
	TOTAL 0010	2,535	2,145	4,290	1,755	14

CATEGORY	STATION	TO	STATION	LOCATION	646.1020 MARKING LINE EPOXY 4-INCH LF	646.7420 MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH LF	646.7520 MARKING CROSSWALK EPOXY BLOCK STYLE 24-INCH LF	REMARKS
0010	101+00	-	133+25	CENTERLINE	6,460	--	--	CENTERLINE, YELLOW
0010	101+00	-	133+25	LT	3,230	--	--	LT, WHITE
0010	101+00	-	133+25	RT	3,230	--	--	RT, WHITE
0010	--	-	--	PINE LODGE RD	--	--	88	
0010	--	-	--	HORLACHER LN	--	90	--	
				TOTAL 0010	12,920	90	88	

CATEGORY	LOCATION	650.4000 CONSTRUCTION STAKING SEWER EACH	650.4500 CONSTRUCTION STAKING SUBGRADE LF	650.5000 CONSTRUCTION STAKING BASE LF	650.5500 CONSTRUCTION STAKING CURB GUTTER AND LF	650.9000 CONSTRUCTION STAKING CURB RAMPS EACH	650.9911.01 CONSTRUCTION STAKING SUPPLEMENTAL CONTROL (PROJECT) (01. 7995-02-65) EACH	650.9920 CONSTRUCTION STAKING SLOPE STAKES LF	650.9500.01 CONSTRUCTION STAKING SIDEWALK (PROJECT) (01. 7995-02-65) EACH
0010	PROJECT	62	3,370	3,370	6,610	6	1	3,370	1
	TOTAL 0010	62	3,370	3,370	6,610	6	1	3,370	1

CATEGORY	STATION	LOCATION	690.0150 SAWING ASPHALT LF	690.0250 SAWING CONCRETE LF
0010	12+00	PINE LODGE RD	50	26
0010	101+00	START PROJECT	40	5
0010	119+10	HORLACHER LN	25	--
0010	133+25	END PROJECT/MENARDS ENTRANCE	87	78
		TOTAL 0010	202	109

*										
CATEGORY	STATION	LOCATION	611.8110 ADJUSTING MANHOLE COVERS EACH	SPV.0060.01 SPECIAL (01. REMOVE & RELOCATE HYDRANT) EACH	SPV.0060.02 SPECIAL (02. SUPPLY & INSTALL HYDRANT) EACH	SPV.0060.03 SPECIAL (03. 6-INCH DUCTILE IRON HYDRANT LEAD) EACH	SPV.0060.04 SPECIAL (04. 6-INCH VALVE & BOX) EACH	SPV.0060.05 SPECIAL (05. ADJUST VALVE & BOX) EACH	SPV.0060.09 SPECIAL (09. REMOVE HYDRANT) EACH	
0020	101+96	ML	--	1	--	48	1	--	--	
0020	128+02	RT	--	--	1	8	--	--	--	
0020	128+31	LT	1	--	--	--	--	--	--	
0020	128+36	LT	--	--	--	--	--	2	1	
0020	131+70	LT	1	--	--	--	--	--	--	
TOTAL 0020			2	1	1	56	1	2	1	

* QUANTITIES SHOWN ELSEHERE IN THE PLAN

CATEGORY	STATION	TO	STATION	LOCATION	601.0411 CONCRETE CURB & GUTTER 30- INCH TYPE D LF	SPV.0090.01 SPECIAL (01. CONCRETE CURB & GUTTER 24- INCH TYPE D) LF
0010	100+05	-	100+25	LT	41	-
0010	101+00	-	133+25	LT	-	3,240
0010	101+00	-	133+25	RT	-	3,250
0010	118+60	-	118+90	LT	-	60
0010	119+25	-	119+60	LT	-	60
0010	133+25	-	133+50	LT	98	-
TOTAL 0010					139	6,610

CONVENTIONAL SYMBOLS

SECTION LINE	--- --	SECTION CORNER SYMBOL		R/W MONUMENT (TO BE SET)	●
QUARTER LINE	--- --	NON-MONUMENTED R/W POINT	○	FOUND IRON PIN (1-INCH UNLESS NOTED)	IP
SIXTEENTH LINE	--- --	SECTION CORNER MONUMENT			
NEW REFERENCE LINE	--- --	GEODETIC SURVEY MONUMENT			
NEW R/W LINE	--- --	SIXTEENTH CORNER MONUMENT			
EXISTING R/W OR HE LINE	--- --	SIGN		OFF-PREMISE SIGN	
PROPERTY LINE	--- --				
LOT, TIE & OTHER MINOR LINES	--- --				
SLOPE INTERCEPT	--- --	COMPENSABLE		NON-COMPENSABLE	
CORPORATE LIMITS	--- --	ELECTRIC POLE		TELEPHONE POLE	
UNDERGROUND FACILITY (COMMUNICATIONS, ELECTRIC, ETC)	--- --	PEDESTAL (LABEL TYPE) (TV, TEL, ELEC, ETC.)			
NEW R/W (FEE OR HE) (HATCHING VARIES BY OWNER)	--- --	ACCESS RESTRICTED BY ACQUISITION			
TEMPORARY LIMITED EASEMENT AREA	--- --	NO ACCESS (BY STATUTORY AUTHORITY)			
EASEMENT AREA (PERMANENT LIMITED OR RESTRICTED DEVELOPMENT)	--- --	ACCESS RESTRICTED (BY PREVIOUS PROJECT OR CONTROL)			
TRANSMISSION STRUCTURES	--- --	NO ACCESS (NEW HIGHWAY)			
BUILDING TO BE REMOVED		PARCEL NUMBER (25)		UTILITY NUMBER (40)	
BRIDGE		PARALLEL OFFSETS			
CULVERT					

CONVENTIONAL UTILITY SYMBOLS			
WATER	--- --		
GAS	--- --		
TELEPHONE	--- --		
OVERHEAD TRANSMISSION LINES	--- --		
ELECTRIC	--- --		
CABLE TELEVISION	--- --		
FIBER OPTIC	--- --		
SANITARY SEWER	--- --		
STORM SEWER	--- --		
ELECTRIC TOWER			
CONVENTIONAL ABBREVIATIONS			
ACCESS RIGHTS	AR	POINT OF COMPOUND CURVE	PCC
ACRES	AC	POINT OF INTERSECTION	PI
AHEAD	AH	PROPERTY LINE	PL
ALUMINUM	ALUM	RECORDED AS	(100')
AND OTHERS	ET AL	REEL / IMAGE	R/I
BACK	BK	REFERENCE LINE	R/L
BLOCK	BLK	REMAINING	REM
CENTERLINE	C/L	RESTRICTIVE DEVELOPMENT EASEMENT	RDE
CERTIFIED SURVEY MAP	CSM		
CONCRETE	CONC	RIGHT	RT
COUNTY	CO	RIGHT OF WAY	R/W
COUNTY TRUNK HIGHWAY	CTH	SECTION	SEC
DISTANCE	DIST	SEPTIC VENT	SEPV
CORNER	COR	SQUARE FEET	SF
DOCUMENT NUMBER	DOC	STATE TRUNK HIGHWAY	STH
EASEMENT	EASE	STATION	STA
EXISTING	EX	TELEPHONE PEDESTAL	TP
GAS VALVE	GV	TEMPORARY LIMITED EASEMENT	TLE
GRID NORTH	GN		
HIGHWAY EASEMENT	HE	TRANSPORTATION PROJECT PLAT	TPP
IDENTIFICATION	ID	UNITED STATES HIGHWAY	USH
LAND CONTRACT	LC	VOLUME	V
LEFT	LT		
MONUMENT	MON		
NATIONAL GEODETIC SURVEY	NGS		
NUMBER	NO		
OUTLOT	OL		
PAGE	P		
POINT OF TANGENCY	PT		
PERMANENT LIMITED EASEMENT	PLE		
POINT OF BEGINNING	POB		
POINT OF CURVATURE	PC		
CURVE DATA ABBREVIATIONS			
		LONG CHORD	LCH
		LONG CHORD BEARING	LCB
		RADIUS	R
		DEGREE OF CURVE	D
		CENTRAL ANGLE	Δ / DELTA
		LENGTH OF CURVE	L
		TANGENT	T
		DIRECTION AHEAD	DA
		DIRECTION BACK	DB

NOTES:
 POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), EAU CLAIRE COUNTY, NAD83 (2011) IN US SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 3/4" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

DIMENSIONING FOR THE NEW RIGHT-OF-WAY IS MEASURED ALONG AND PERPENDICULAR TO THE NEW REFERENCE LINES.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS" OF PUBLIC RECORD.

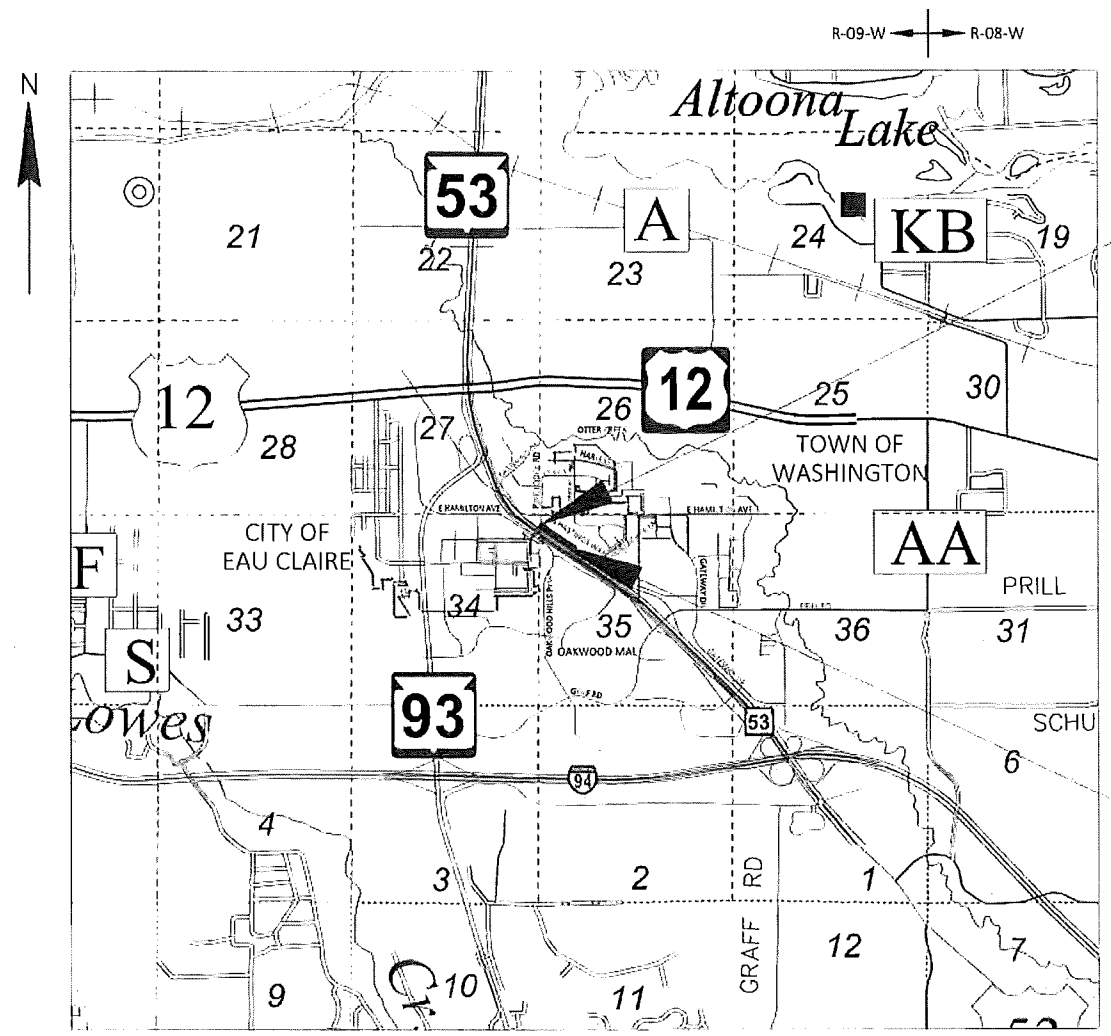
A TEMPORARY LIMITED EASEMENT (TLE) IS A RIGHT FOR CONSTRUCTION PURPOSES, AS DEFINED HEREIN, INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON, THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM DESIRABLE. ALL (TLEs) ON THIS PLAT EXPIRE AT THE COMPLETION OF THE CONSTRUCTION PROJECT FOR WHICH THIS INSTRUMENT IS GIVEN.

PARCEL AND UTILITY IDENTIFICATION NUMBERS MAY NOT POINT TO ALL AREAS OF ACQUISITION, AS NOTED ON THE DETAIL PAGES.

INFORMATION FOR THE BASIS OF EXISTING HIGHWAY RIGHT-OF-WAY POINTS OF REFERENCE AND ACCESS CONTROL ARE LISTED ON THE DETAIL PAGES.

THIS PLAT IS A GRAPHIC REPRESENTATION AND IS FOR REFERENCE PURPOSES ONLY. DEEDS MUST BE CHECKED TO DETERMINE PROPERTY BOUNDARIES AND ACCESS RIGHTS.

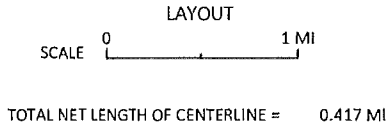
R/W PROJECT NUMBER 7995-02-64	SHEET NUMBER 4.01	TOTAL SHEETS 5
FEDERAL PROJECT NUMBER N/A		
PLAT OF RIGHT OF WAY REQUIRED FOR C EAU CLAIRE, S HASTINGS WAY GATEWAY DRIVE TO PINE LODGE ROAD		
LOCAL STREET	EAU CLAIRE COUNTY	
CONSTRUCTION PROJECT NUMBER 7995-02-65		



BEGIN RELOCATION ORDER
 STA 100+00
 293.27' SOUTH AND 51.48' WEST OF THE NORTHWEST CORNER OF SEC 35, T-27-N, R-9-W



END RELOCATION ORDER
 STA 122+00
 1,526.13' SOUTH AND 873.10' WEST OF THE NORTH QUARTER CORNER OF SEC 35, T-27-N, R-9-W

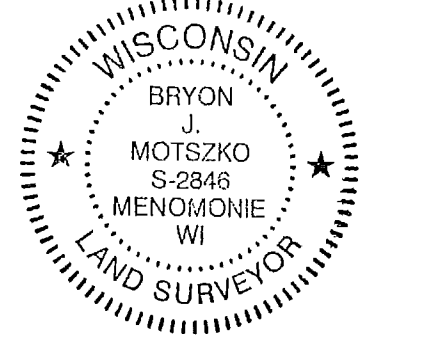


CORRE



ENGINEERING

MADISON | EAU CLAIRE | WAUKESHA | APPLETON
 TOMAH | WITTENBERG



I, BRYON J. MOTSZKO, PROFESSIONAL LAND SURVEYOR, S-2846, HEREBY CERTIFY THAT I HAVE SURVEYED THE LAND DESCRIBED HEREON AND THAT THE MAP HEREON IS A CORRECT REPRESENTATION OF THAT SURVEY TO THE BEST OF MY KNOWLEDGE AND BELIEF.

DATE: 5/20/22 *Bryon J. Motszko*
 (Signature)

REVISION DATE
9/27/2022
12/7/2022

CITY OF EAU CLAIRE

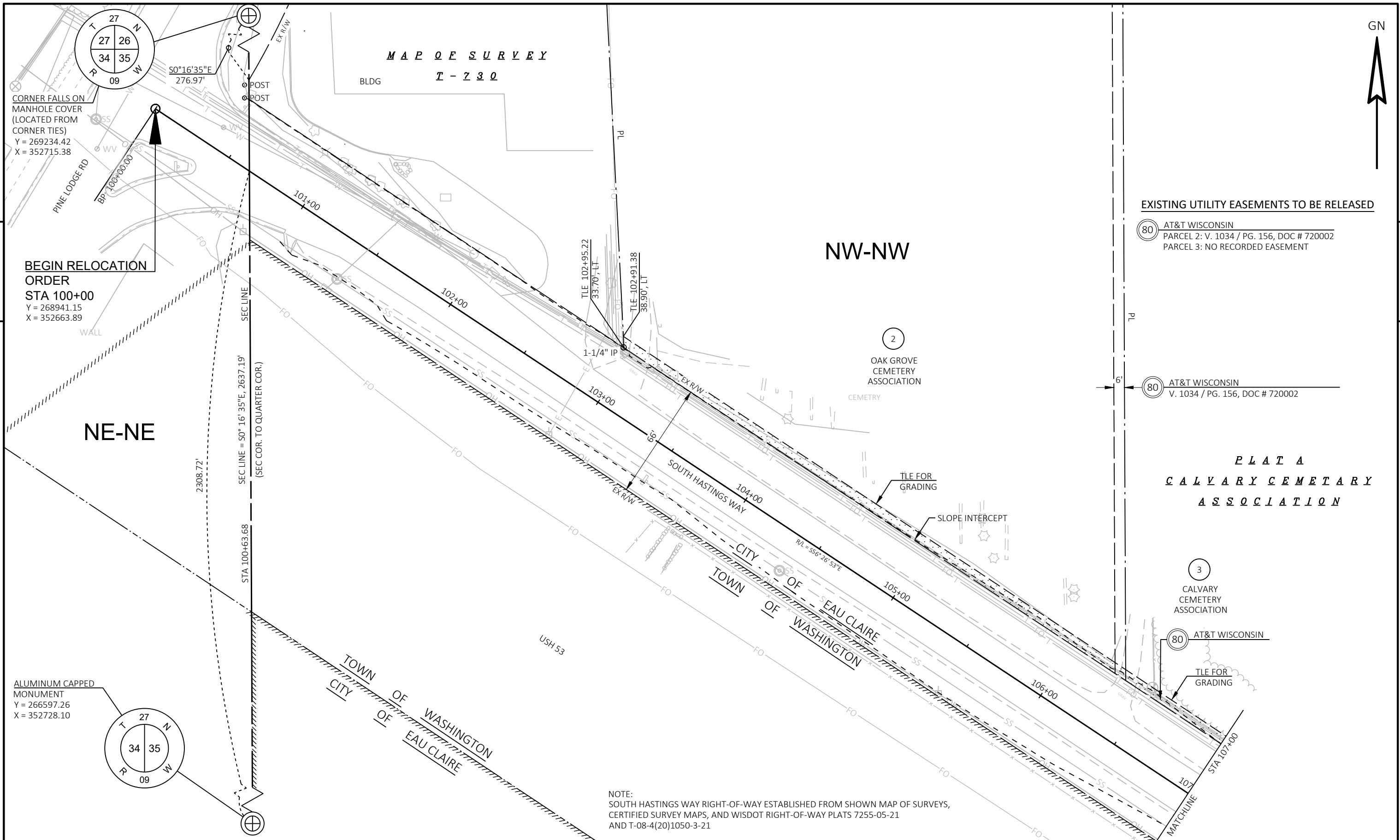
APPROVED FOR THE CITY
 DATE: 5/23/22 *[Signature]*
 (Signature)

SCHEDULE OF LANDS & INTERESTS REQUIRED

OWNERS NAMES ARE SHOWN FOR REFERENCE ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE CITY.

PARCEL NUMBER	SHEET NUMBER	OWNER	INTEREST REQUIRED	RIGHT OF WAY REQUIRED			TLE REQUIRED	PLE REQUIRED
				NEW (AC)	EXISTING (AC)	TOTAL (AC)	TOTAL (AC)	TOTAL (AC)
2	4.03	OAK GROVE CEMETERY ASSOCIATION	TLE	---	---	---	0.04	---
3	4.03 - 4.05	CALVARY CEMETERY ASSOCIATION	TLE	---	---	---	0.12	---
4	4.05	WAYNE R. PETERS	TLE	---	---	---	0.02	---
5	4.05	EAU CLAIRE CONGREGATION OF JEHOVAH'S WITNESSES	TLE	---	---	---	0.05	---
80	4.03 - 4.05	AT&T WISCONSIN		RELEASE OF RIGHTS				
81	4.04 - 4.05	XCEL ENERGY		RELEASE OF RIGHTS				
82	4.04 - 4.05	CHARTER COMMUNICATIONS		RELEASE OF RIGHTS				

REVISION DATE 9/27/2022 12/7/2022	DATE 05/20/2022	SCALE, FEET 0 _____ N/A	HWY: LOCAL STREET	STATE R/W PROJECT NUMBER: 7995-02-64	PLAT SHEET 4.02
GRID FACTOR _____	COUNTY: EAU CLAIRE	CONSTRUCTION PROJECT NUMBER: 7995-02-65	PS&E SHEET _____	E	



CORNER FALLS ON MANHOLE COVER (LOCATED FROM CORNER TIES)
 Y = 269234.42
 X = 352715.38

BEGIN RELOCATION ORDER
 STA 100+00
 Y = 268941.15
 X = 352663.89

ALUMINUM CAPPED MONUMENT
 Y = 266597.26
 X = 352728.10

MAP OF SURVEY
 T - 7 3 0

EXISTING UTILITY EASEMENTS TO BE RELEASED

(80) AT&T WISCONSIN
 PARCEL 2: V. 1034 / PG. 156, DOC # 720002
 PARCEL 3: NO RECORDED EASEMENT

(80) AT&T WISCONSIN
 V. 1034 / PG. 156, DOC # 720002

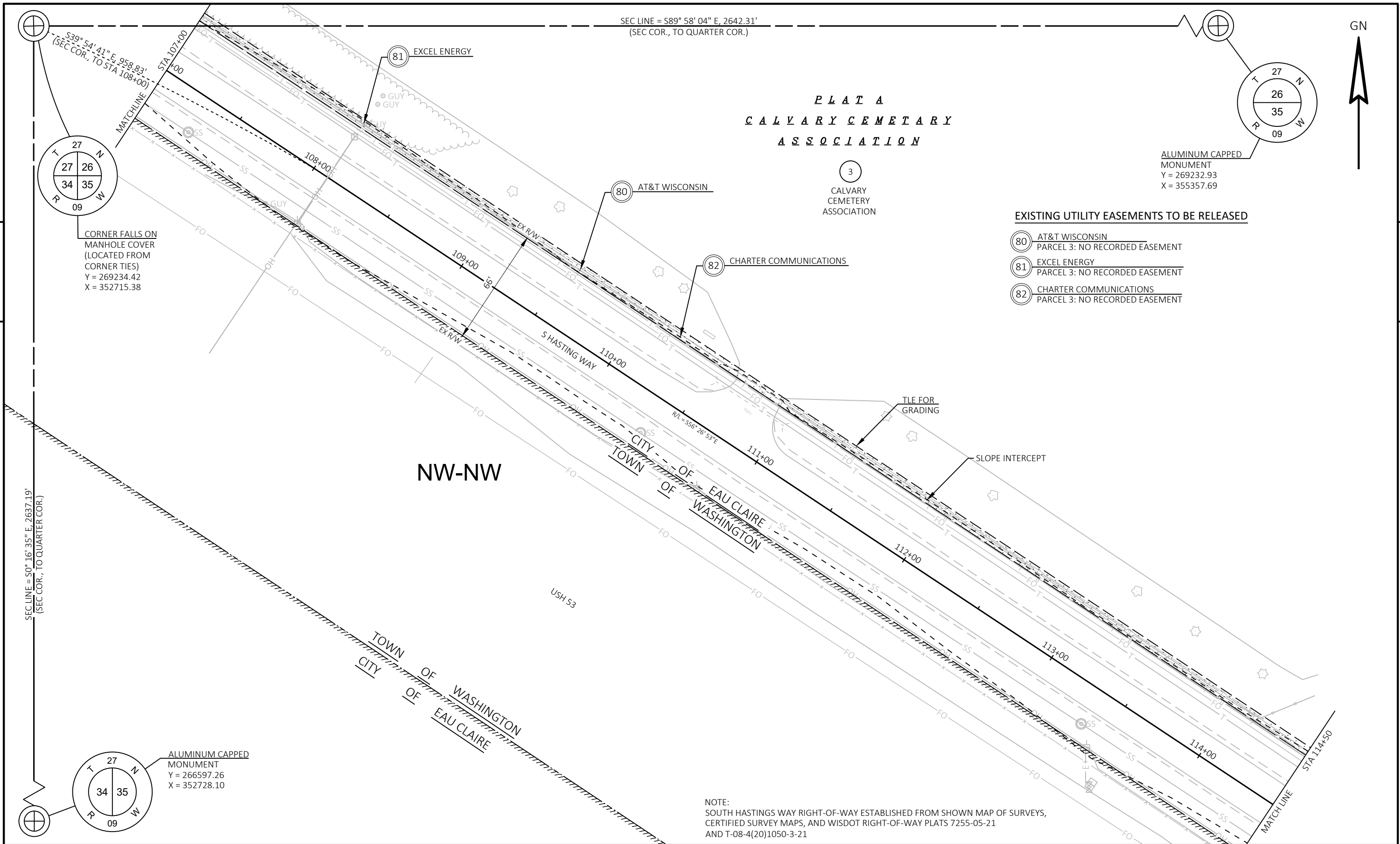
(3) CALVARY CEMETERY ASSOCIATION

(80) AT&T WISCONSIN

TLE FOR GRADING

NOTE:
 SOUTH HASTINGS WAY RIGHT-OF-WAY ESTABLISHED FROM SHOWN MAP OF SURVEYS,
 CERTIFIED SURVEY MAPS, AND WISDOT RIGHT-OF-WAY PLATS 7255-05-21
 AND T-08-4(20)1050-3-21

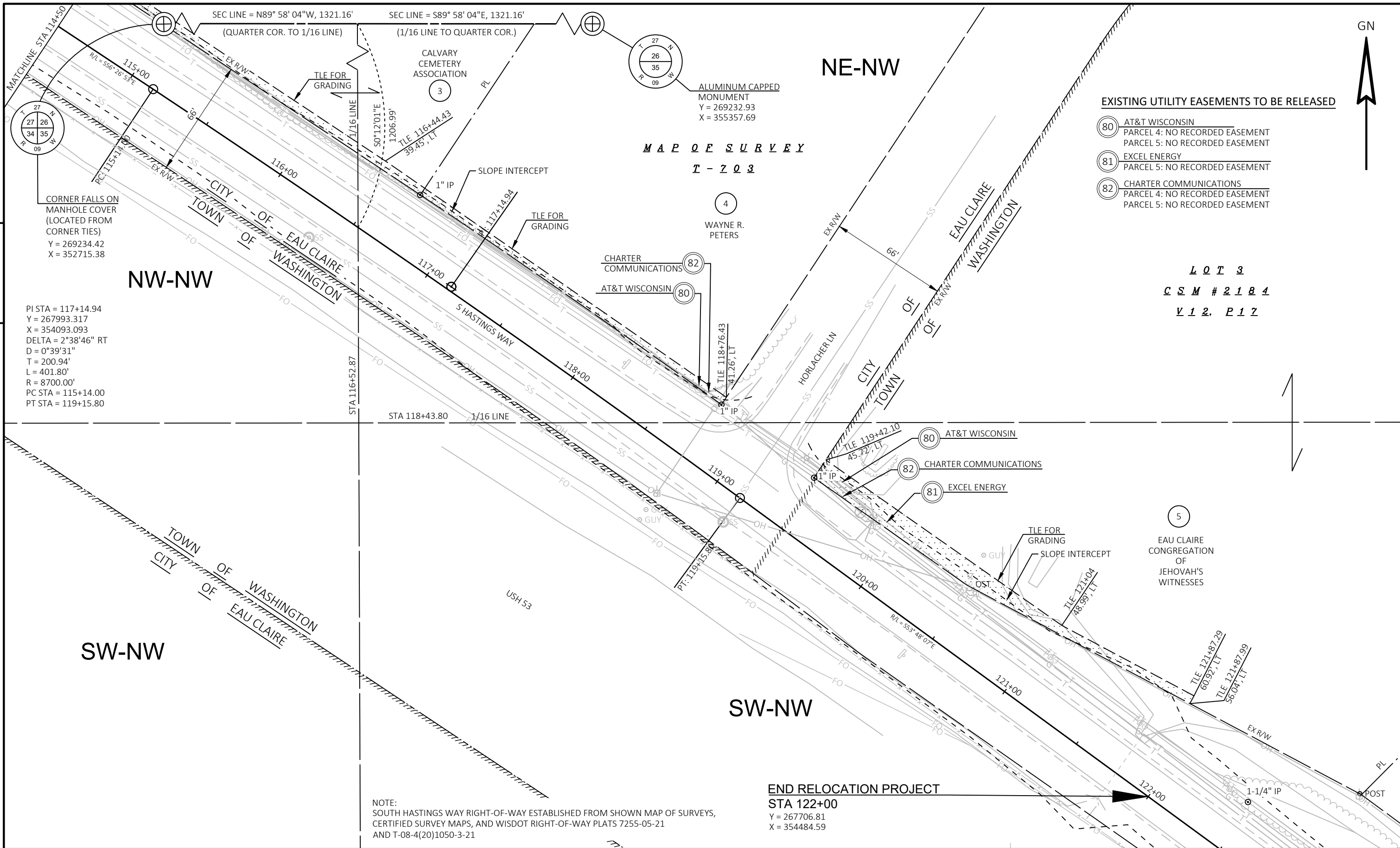
REVISION DATE 9/27/2022 12/7/2022	DATE 05/20/2022	SCALE, FEET 0 25 50	HWY: LOCAL STREET	STATE R/W PROJECT NUMBER: 7995-02-64	PLAT SHEET 4.03
GRID FACTOR			COUNTY: EAU CLAIRE	CONSTRUCTION PROJECT NUMBER: 7995-02-65	PS&E SHEET



4

4

REVISION DATE 9/27/2022 - N.C. 12/7/2022 - N.C.	DATE 05/20/2022	SCALE, FEET 0 25 50	HWY: LOCAL STREET	STATE R/W PROJECT NUMBER: 7995-02-64	PLAT SHEET 4.04
GRID FACTOR			COUNTY: EAU CLAIRE	CONSTRUCTION PROJECT NUMBER: 7995-02-65	PS&E SHEET



EXISTING UTILITY EASEMENTS TO BE RELEASED

- (80) AT&T WISCONSIN
PARCEL 4: NO RECORDED EASEMENT
PARCEL 5: NO RECORDED EASEMENT
- (81) EXCEL ENERGY
PARCEL 5: NO RECORDED EASEMENT
- (82) CHARTER COMMUNICATIONS
PARCEL 4: NO RECORDED EASEMENT
PARCEL 5: NO RECORDED EASEMENT

LOT 3
C S M # 2 1 8 4
V 1 2 . P 1 7

NOTE:
SOUTH HASTINGS WAY RIGHT-OF-WAY ESTABLISHED FROM SHOWN MAP OF SURVEYS,
CERTIFIED SURVEY MAPS, AND WISDOT RIGHT-OF-WAY PLATS 7255-05-21
AND T-08-4(20)1050-3-21

END RELOCATION PROJECT

STA 122+00
Y = 267706.81
X = 354484.59

4

4

REVISION DATE	9/27/2022 - N.C.		
	12/7/2022 - N.C.		

DATE	05/20/2022
GRID FACTOR	

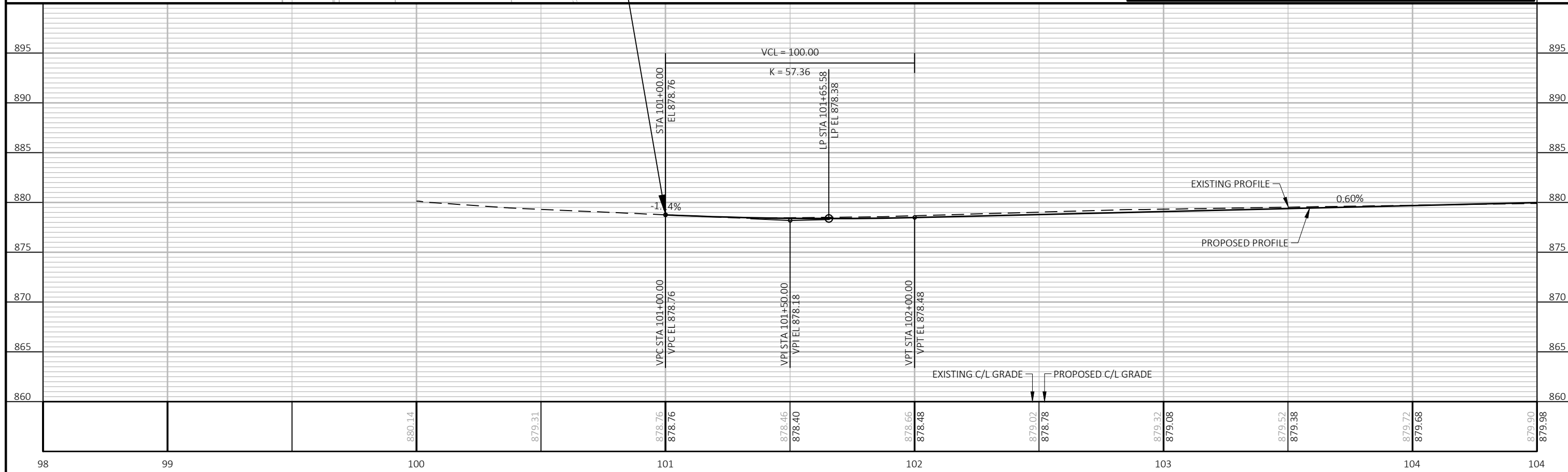
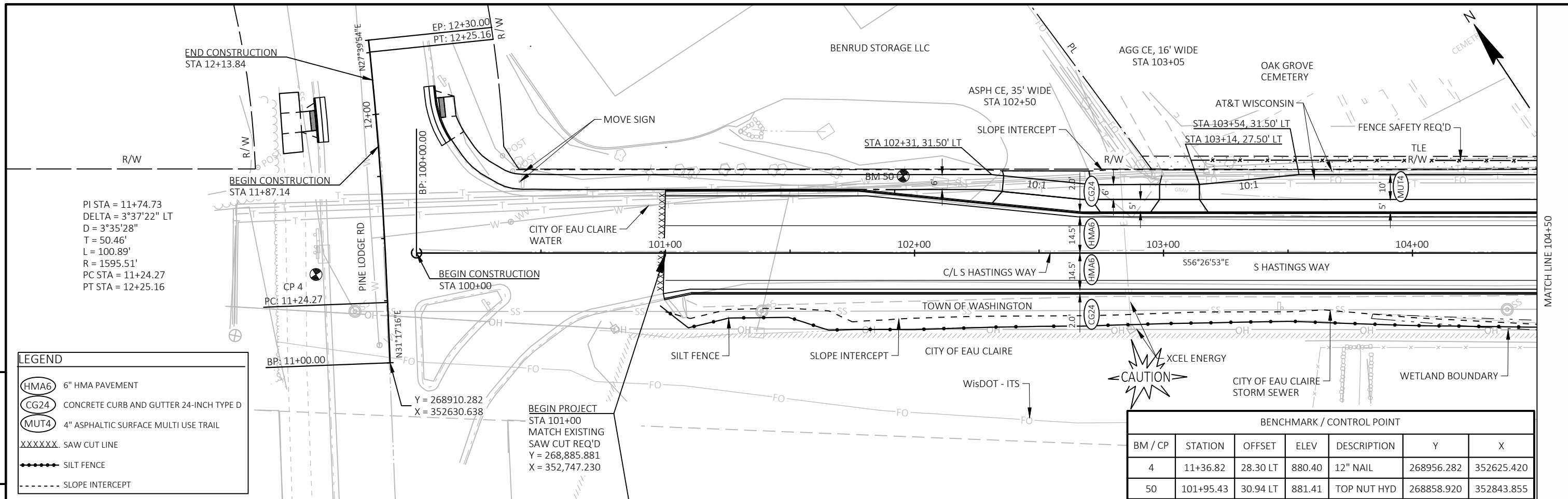


HWY: LOCAL STREET
COUNTY: EAU CLAIRE

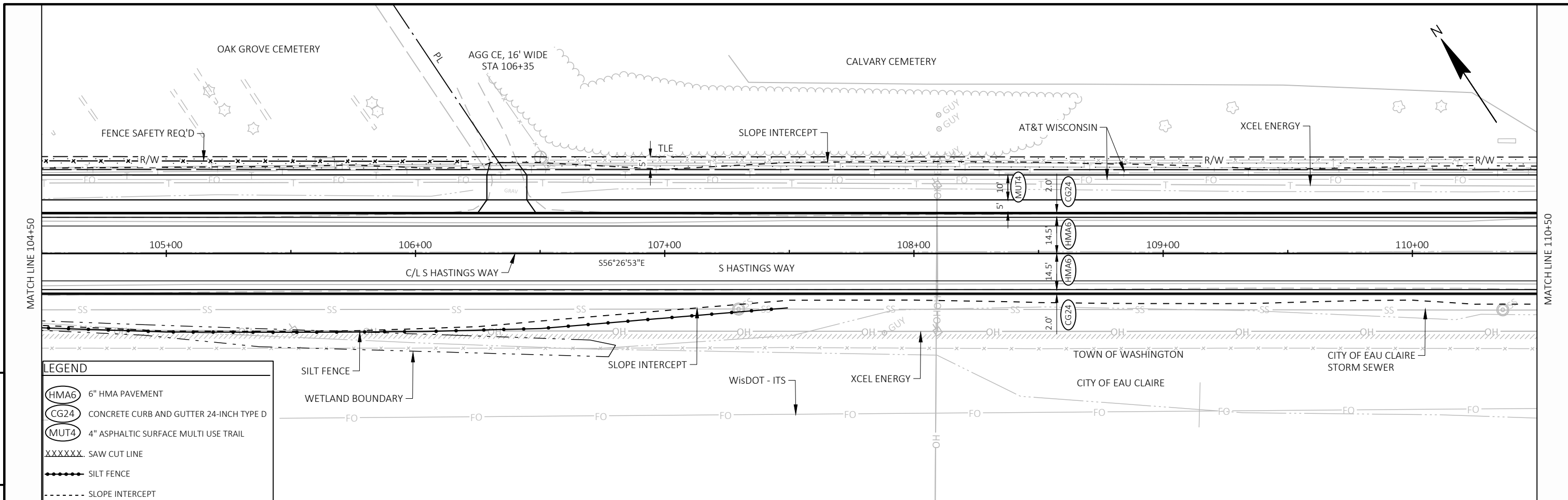
STATE R/W PROJECT NUMBER: 7995-02-64
CONSTRUCTION PROJECT NUMBER: 7995-02-65

PLAT SHEET 4.05
PS&E SHEET

E

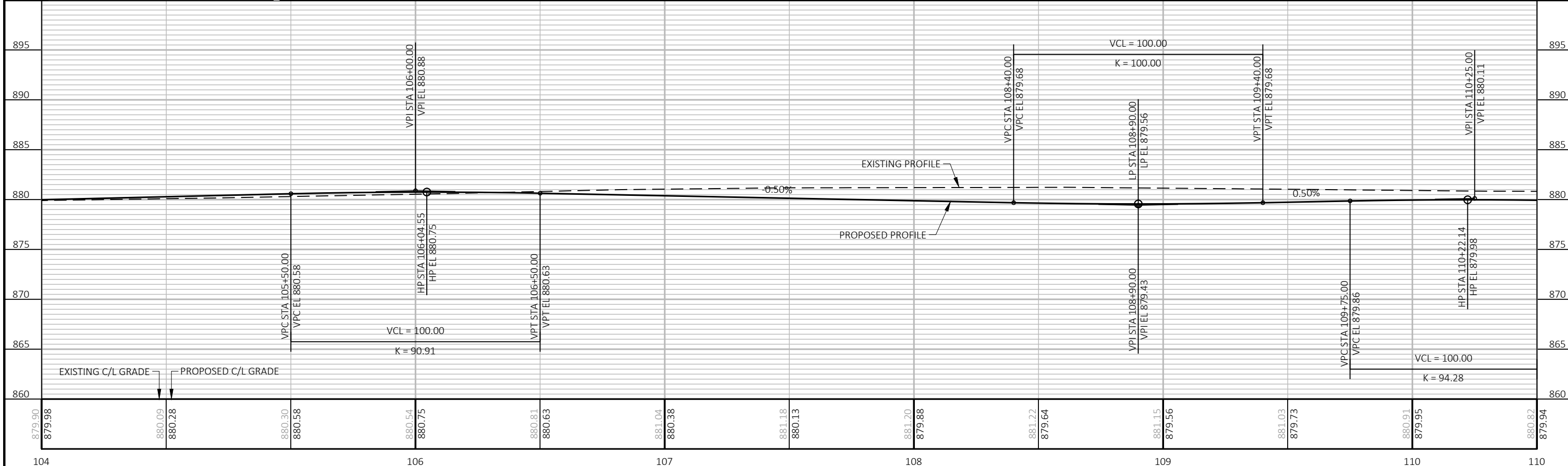


PROJECT NO: 7995-02-65 HWY: S HASTINGS WAY COUNTY: EAU CLAIRE PLAN AND PROFILE: S HASTINGS WAY SHEET E



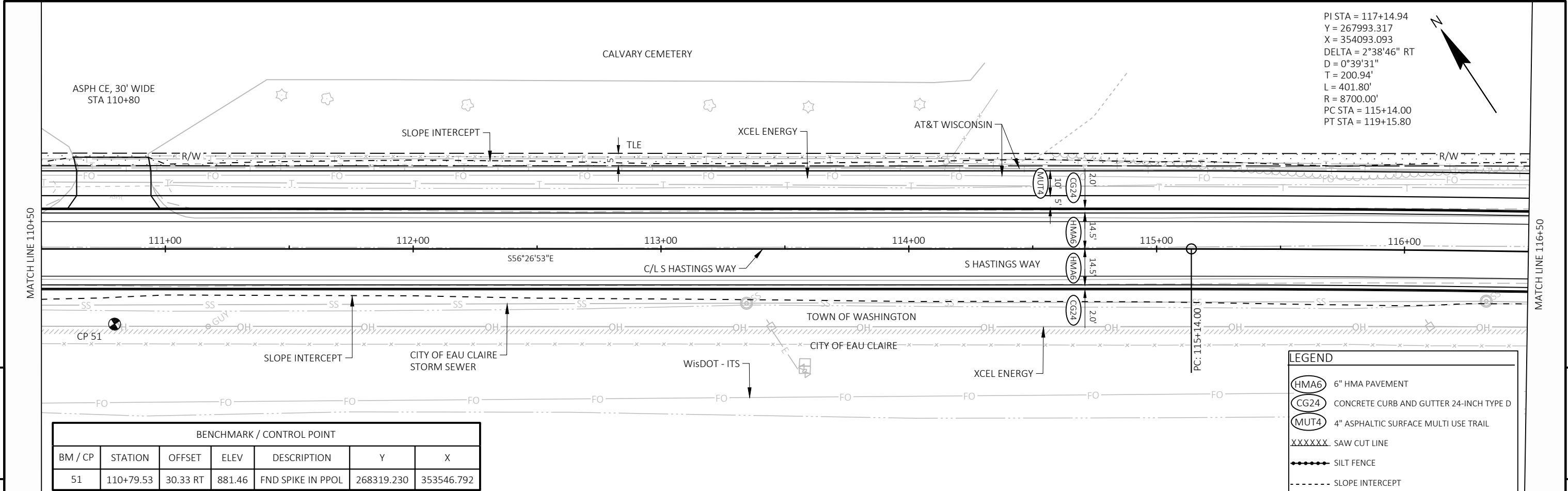
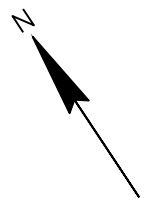
LEGEND

- HMA6 6" HMA PAVEMENT
- CG24 CONCRETE CURB AND GUTTER 24-INCH TYPE D
- MUT4 4" ASPHALTIC SURFACE MULTI USE TRAIL
- XXXXXX SAW CUT LINE
- SILT FENCE
- SLOPE INTERCEPT



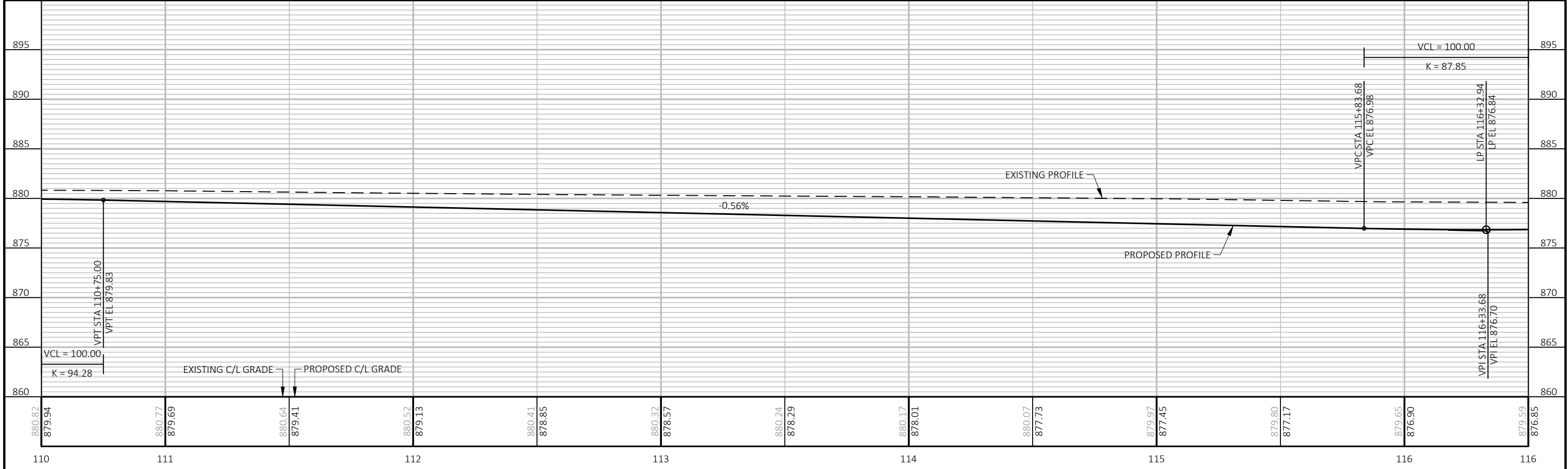
PROJECT NO: 7995-02-65	HWY: S HASTINGS WAY	COUNTY: EAU CLAIRE	PLAN AND PROFILE: S HASTINGS WAY	SHEET	E
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PI STA = 117+14.94
 Y = 267993.317
 X = 354093.093
 DELTA = 2°38'46" RT
 D = 0°39'31"
 T = 200.94'
 L = 401.80'
 R = 8700.00'
 PC STA = 115+14.00
 PT STA = 119+15.80

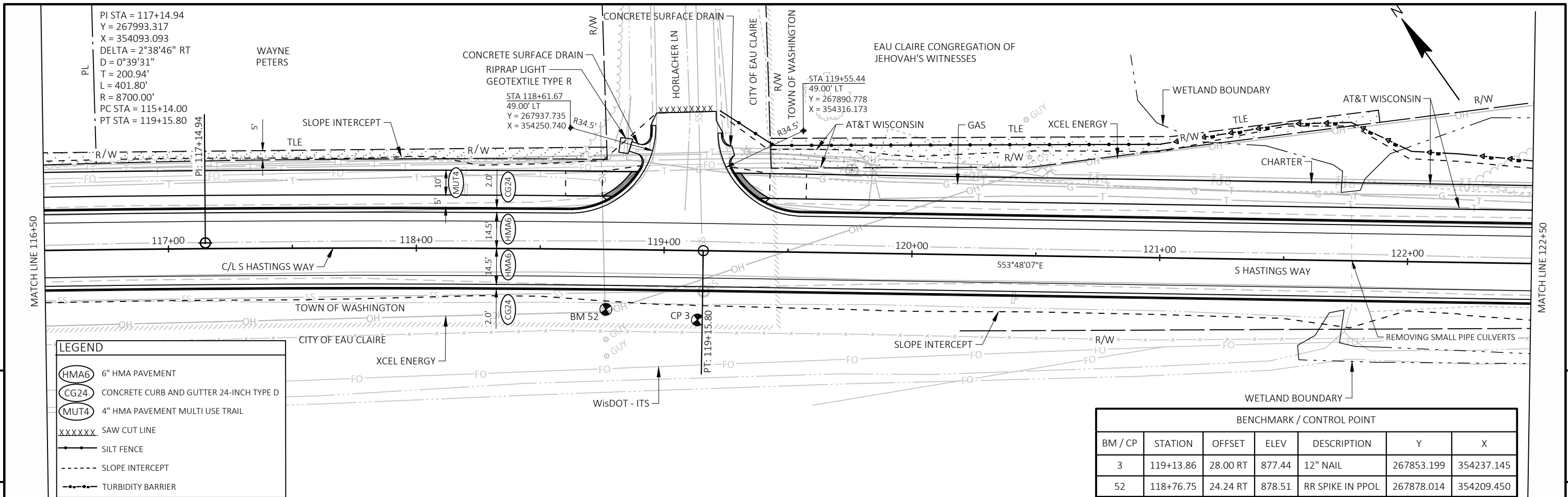


BENCHMARK / CONTROL POINT						
BM / CP	STATION	OFFSET	ELEV	DESCRIPTION	Y	X
51	110+79.53	30.33 RT	881.46	FND SPIKE IN PPOL	268319.230	353546.792

LEGEND	
	6" HMA PAVEMENT
	CONCRETE CURB AND GUTTER 24-INCH TYPE D
	4" ASPHALTIC SURFACE MULTI USE TRAIL
XXXXXX	SAW CUT LINE
	SILT FENCE
	SLOPE INTERCEPT



PROJECT NO: 7995-02-65	HWY: S HASTINGS WAY	COUNTY: EAU CLAIRE	PLAN AND PROFILE: S HASTINGS WAY	SHEET	E
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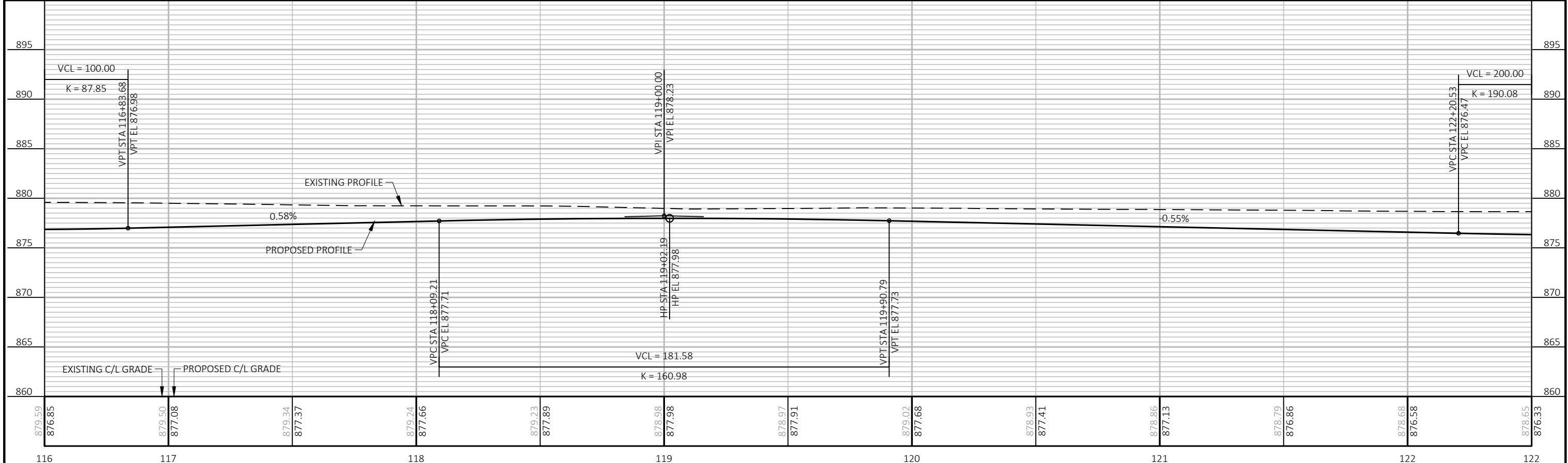


LEGEND

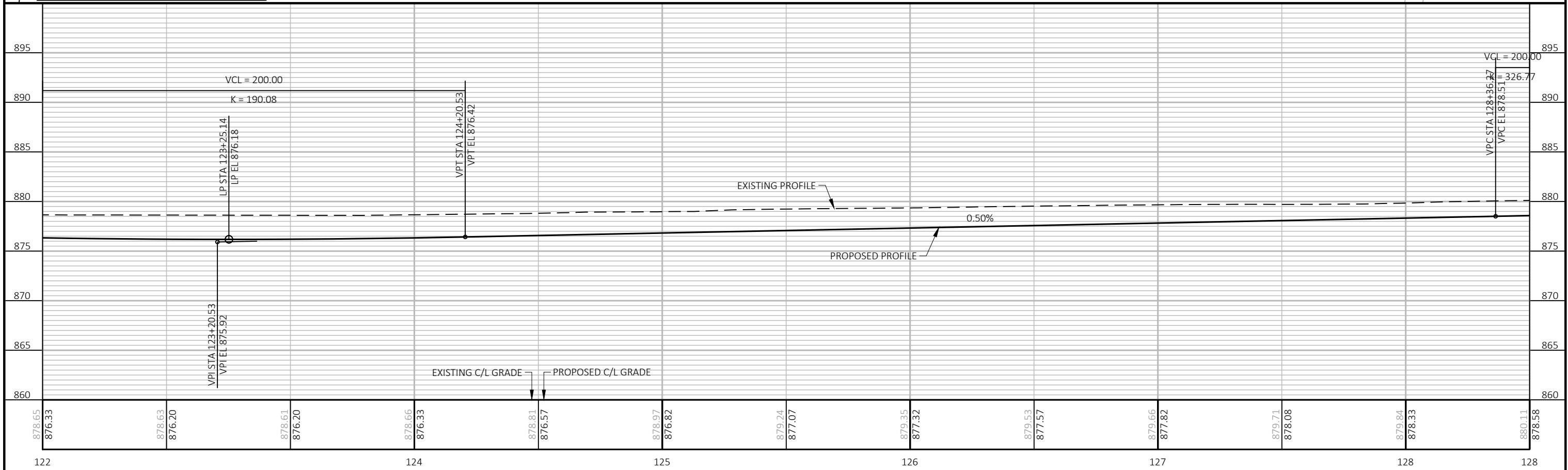
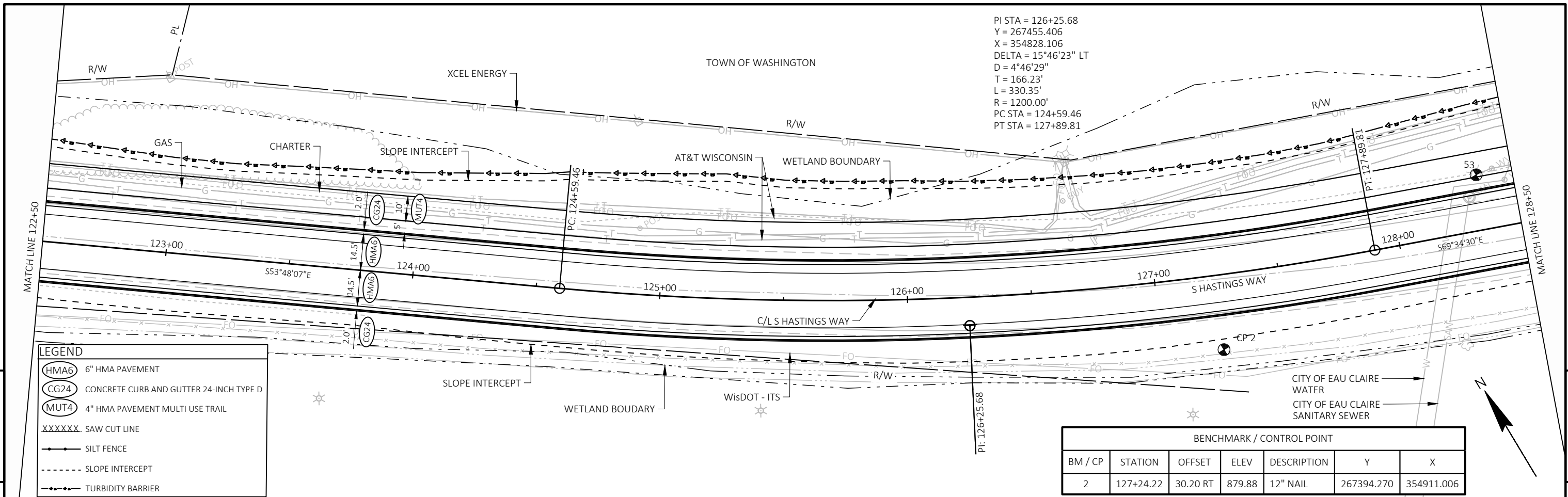
- 6" HMA PAVEMENT
- CONCRETE CURB AND GUTTER 24-INCH TYPE D
- 4" HMA PAVEMENT MULTI USE TRAIL
- SAW CUT LINE
- SILT FENCE
- SLOPE INTERCEPT
- TURBIDITY BARRIER

BENCHMARK / CONTROL POINT

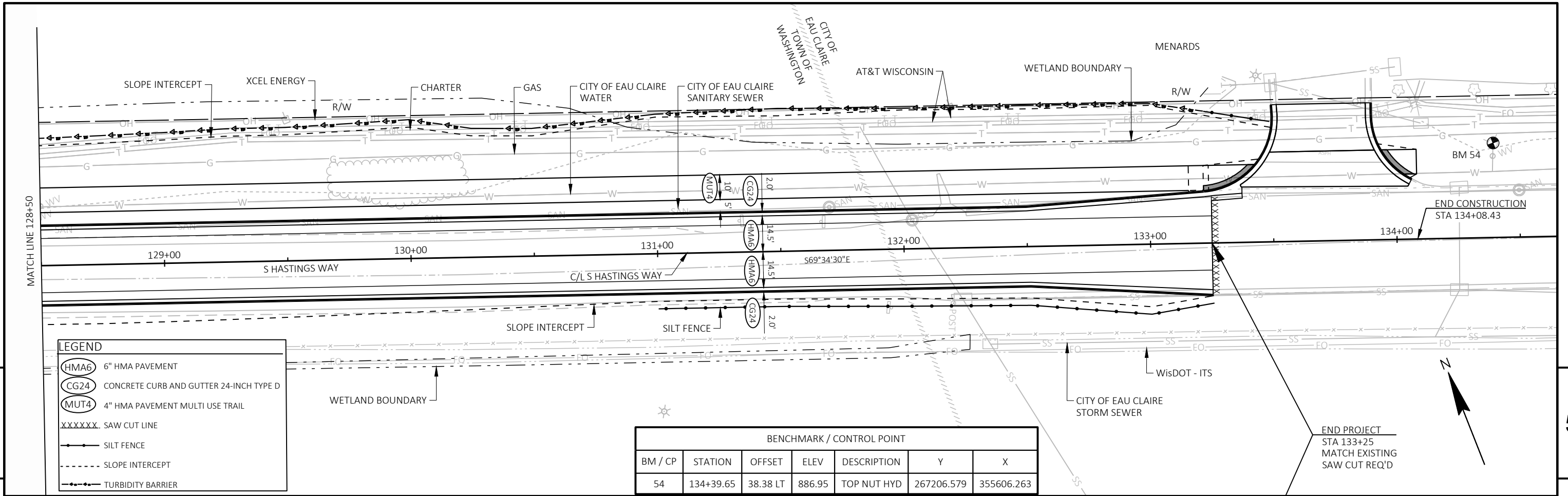
BM / CP	STATION	OFFSET	ELEV	DESCRIPTION	Y	X
3	119+13.86	28.00 RT	877.44	12" NAIL	267853.199	354237.145
52	118+76.75	24.24 RT	878.51	RR SPIKE IN PPOL	267878.014	354209.450



PROJECT NO: 7995-02-65
 HWY: S HASTINGS WAY
 COUNTY: EAU CLAIRE
 PLAN AND PROFILE: S HASTINGS WAY
 SHEET: E



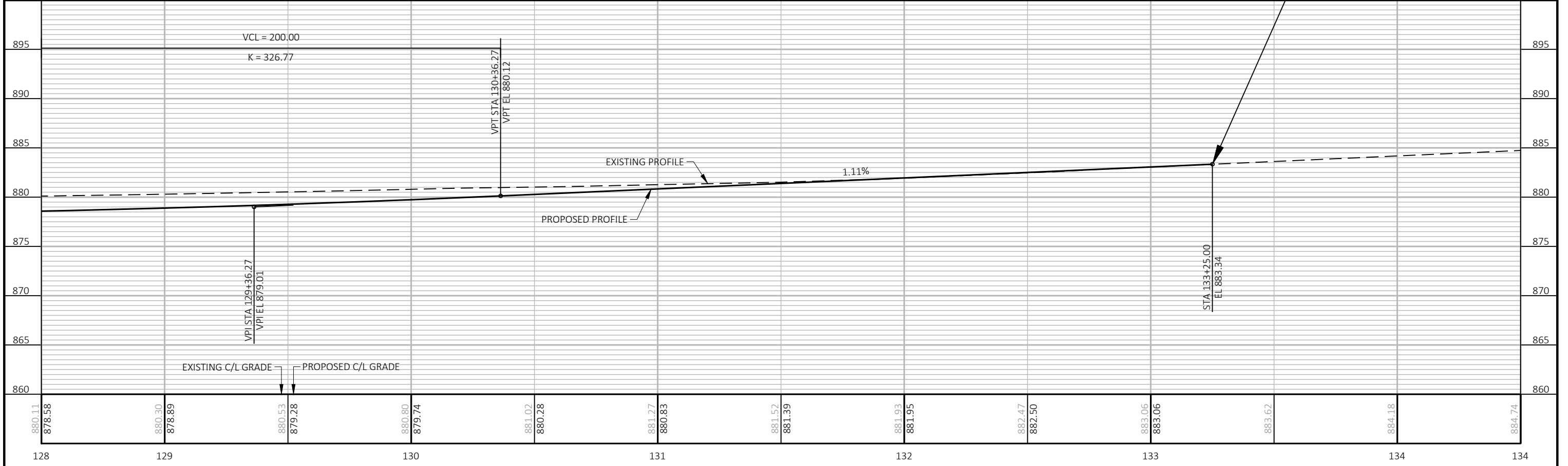
PROJECT NO: 7995-02-65 HWY: S HASTINGS WAY COUNTY: EAU CLAIRE PLAN AND PROFILE: S HASTINGS WAY SHEET E



LEGEND

- 6" HMA PAVEMENT
- CONCRETE CURB AND GUTTER 24-INCH TYPE D
- 4" HMA PAVEMENT MULTI USE TRAIL
- XXXXXX SAW CUT LINE
- SILT FENCE
- SLOPE INTERCEPT
- TURBIDITY BARRIER

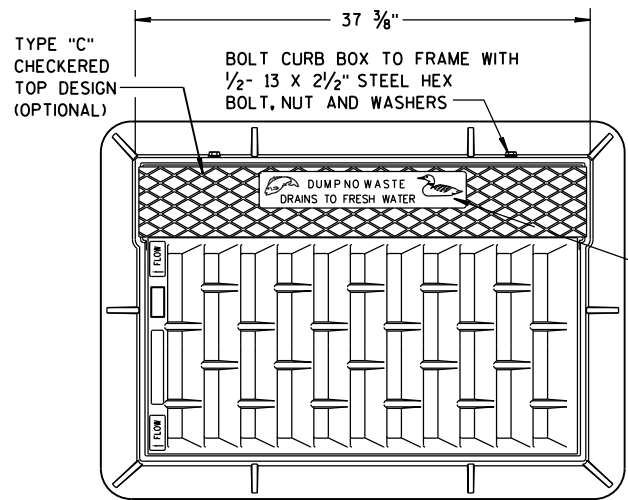
BENCHMARK / CONTROL POINT						
BM / CP	STATION	OFFSET	ELEV	DESCRIPTION	Y	X
54	134+39.65	38.38 LT	886.95	TOP NUT HYD	267206.579	355606.263



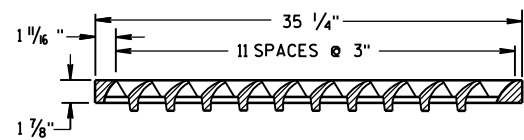
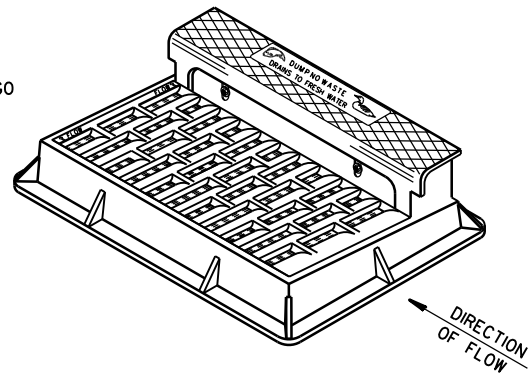
PROJECT NO: 7995-02-65 HWY: S HASTINGS WAY COUNTY: EAU CLAIRE PLAN AND PROFILE: S HASTINGS WAY SHEET **E**

Standard Detail Drawing List

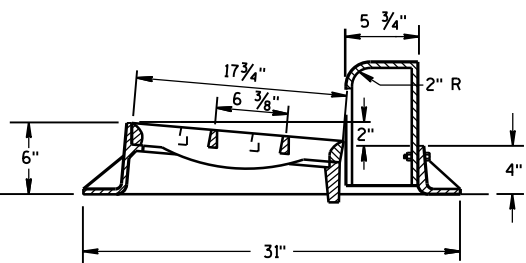
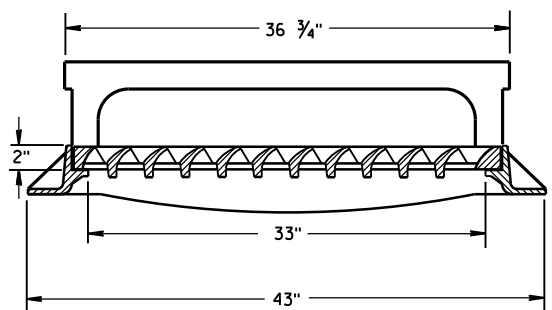
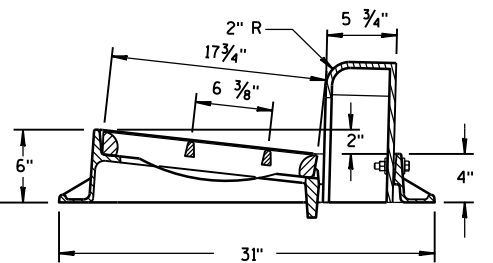
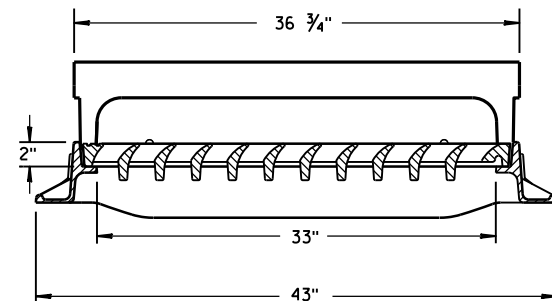
08A05-19A	INLET COVERS TYPE A, H, A-S, H-S & Z
08A05-19D	INLET COVER TYPE BW, MANHOLE COVERS, TYPE K, J, J-S, L & M
08A08-02	CATCH BASINS 3-FT, 4-FT, 5-FT AND 6-FT DIAMETER
08B09-03	MANHOLES 3-FT, 4-FT, 5-FT, 6-FT, 7-FT, 8-FT, 9-FT, 10-FT DIAMETER
08C07-02	INLETS 2X2-FT, 2X2.5-FT, 2X3-FT AND 2.5X3-FT
08D01-22A	CONCRETE CURB & GUTTER
08D04-06	CONCRETE SURFACE DRAINS & ASPHALTIC FLUMES
08D05-20A	CURB RAMPS TYPES 1 AND 1-A
08D05-20B	CURB RAMPS TYPES 2 AND 3
08D05-20C	CURB RAMPS TYPES 4A AND 4A1
08D05-20D	CURB RAMPS TYPE 4B AND 4B1
08D05-20E	CURB RAMPS TYPES 5, 6, 7A, 7B & 8
08D05-20F	CURB RAMPS RADIAL DETECTABLE WARNING FIELD APPLICATIONS
08D05-20G	CURB RAMPS RECTANGULAR AND RADIAL DETECTABLE WARNING PLATES
08D17-06	MANHOLES, MANHOLE & INLET COVERS
08E09-06	SILT FENCE
08E10-02	INLET PROTECTION TYPE A, B, C AND D
08E11-02	TURBIDITY BARRIER
08F04-08	JOINT TIES FOR CONCRETE PIPE AND CONCRETE COLLAR DETAIL
08F10-02	CONCRETE MASONRY ENDWALLS FOR CULVERT PIPE AND PIPE ARCH
15C02-08A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-08B	BARRICADES AND SIGNS FOR VARIOUS CLOSURES
15C03-05	BARRICADES AND SIGNS FOR SIDEROAD CLOSURES
15C08-22A	LONGITUDINAL MARKING (MAINLINE)
15C35-05A	PAVEMENT MARKING (INTERSECTIONS)



**NOTE:
GRATE IS REVERSIBLE.**

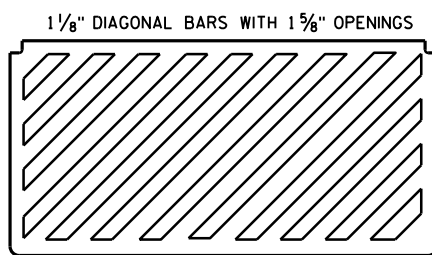


NOTE: CURB BOX HEIGHT ADJUSTABLE 6" TO 9"

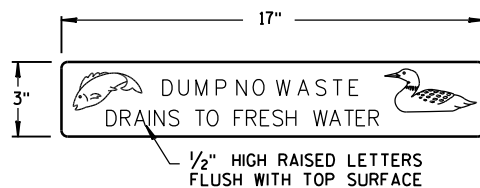


TYPE "H"

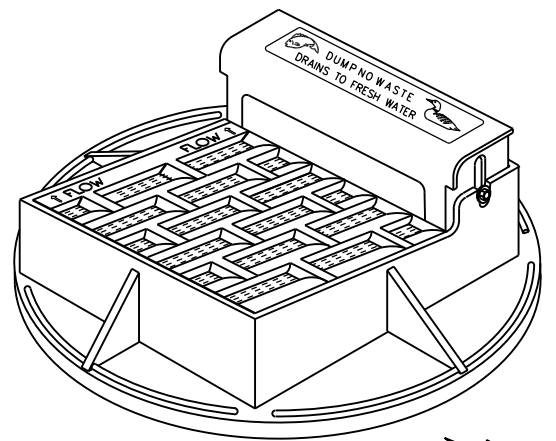
NOTE: EITHER CASTING IS ACCEPTABLE



**SPECIAL GRATE FOR
TYPE "H" COVER**
(MEASURES 35 1/4" X 17 3/4" X 2")
(NOTED AS TYPE H-S ON DRAINAGE TABLE)

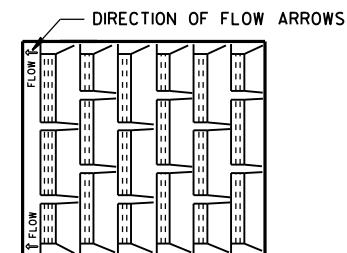


LOGO DETAIL

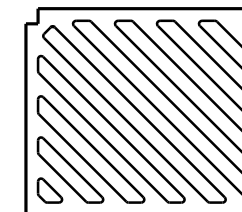


NOTE: CURB BOX ADJUSTABLE 4" TO 9"

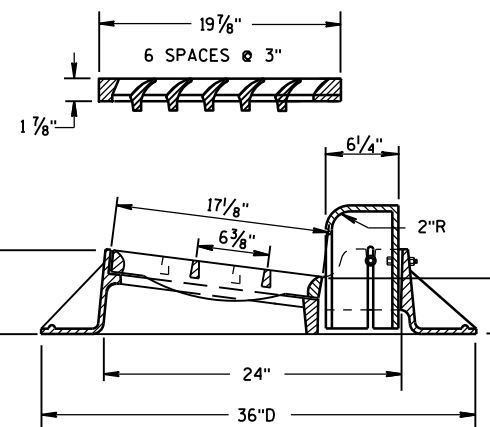
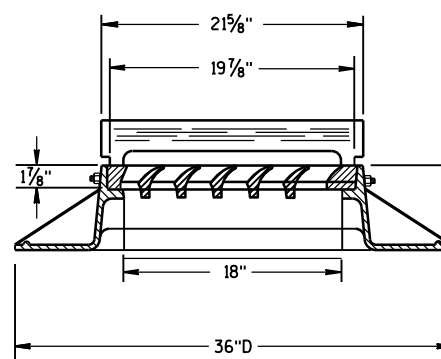
**NOTE:
GRATE IS REVERSIBLE.**



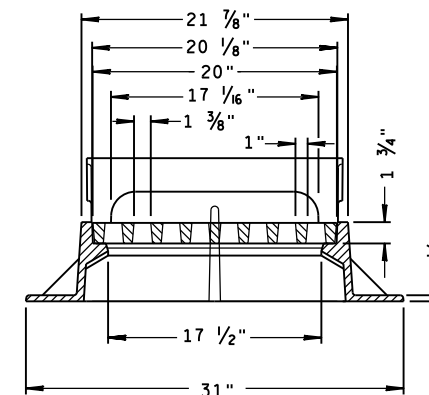
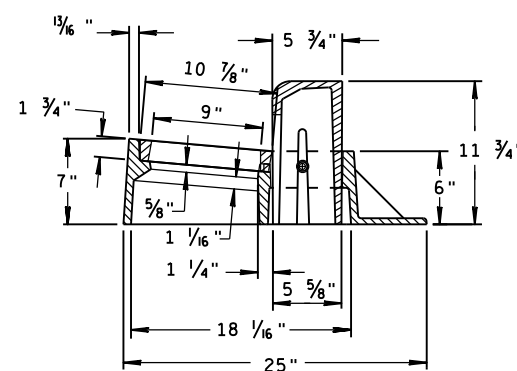
**1" DIAGONAL BARS
WITH 1 1/2" OPENINGS**



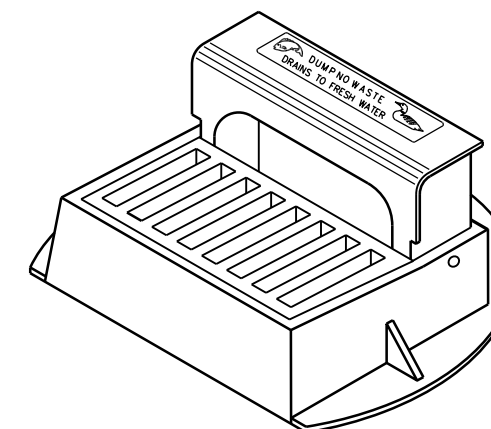
**SPECIAL GRATE FOR
TYPE "A" COVER**
(MEASURES 19 3/4" X 17" X 1 1/8")
(NOTED AS TYPE A-S ON DRAINAGE TABLE)



TYPE "A"



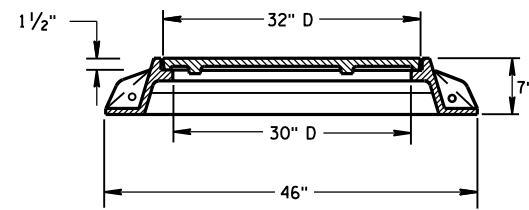
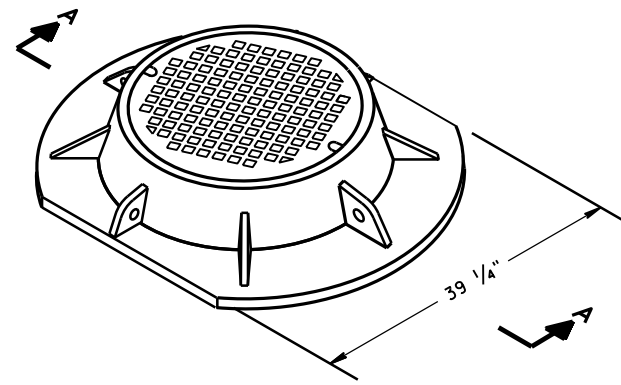
TYPE "Z"



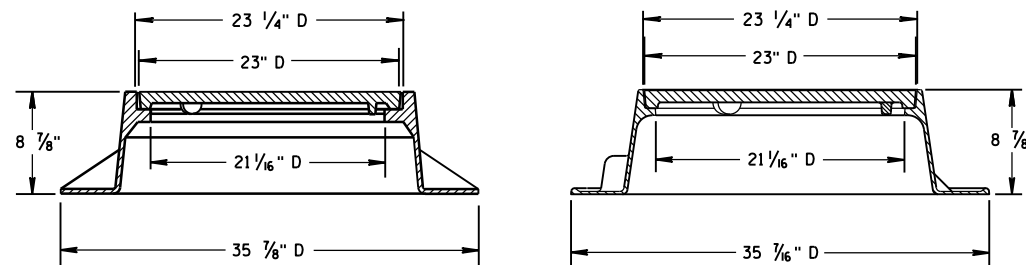
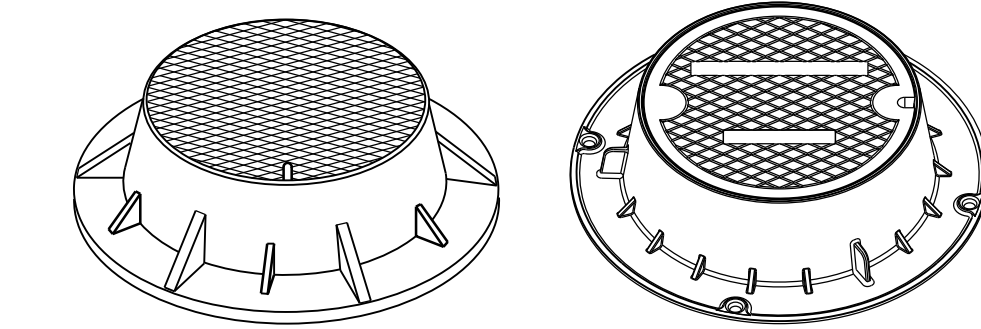
**INLET COVERS
TYPE A, H, A-S, H-S & Z**

**STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION**

APPROVED
11-27-13
DATE /S/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT
ENGINEER
FHWA



SECTION A-A
TYPE "K"



TYPE "J"

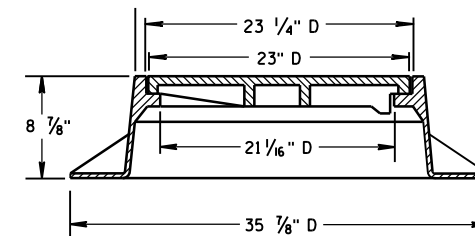
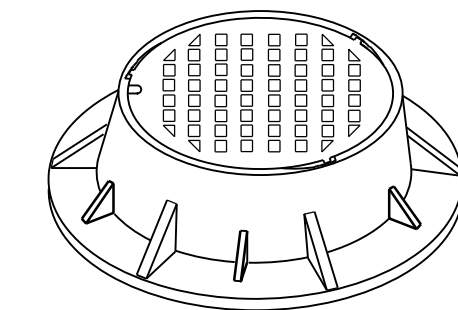
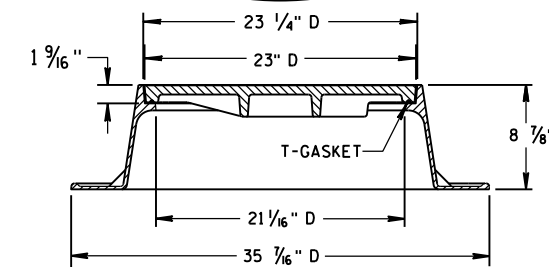
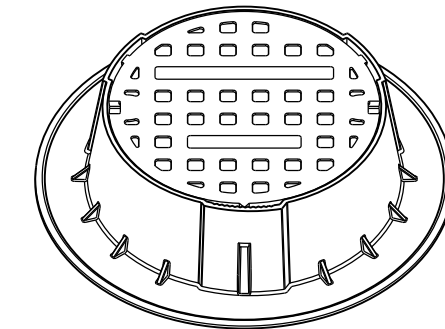
NOTE: EITHER CASTING IS ACCEPTABLE

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

DETAIL DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR MANHOLE COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ROUND FRAMES AND COVERS SHALL HAVE CONTINUOUSLY MACHINED BEARING SURFACES TO PREVENT ROCKING AND RATTLING.



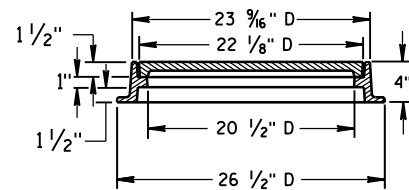
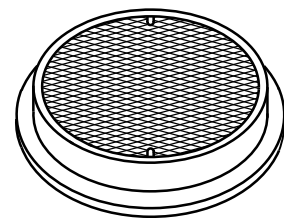
TYPE "J" SPECIAL

TYPE "B" NON-ROCKING SELF-SEAL LID

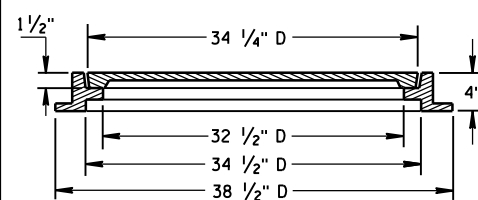
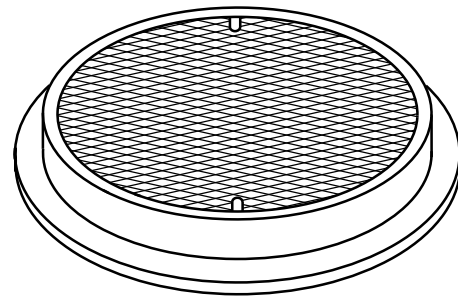
(NOTED AS TYPE J-S ON THE DRAINAGE TABLE)

NOTE: EITHER CASTING IS ACCEPTABLE

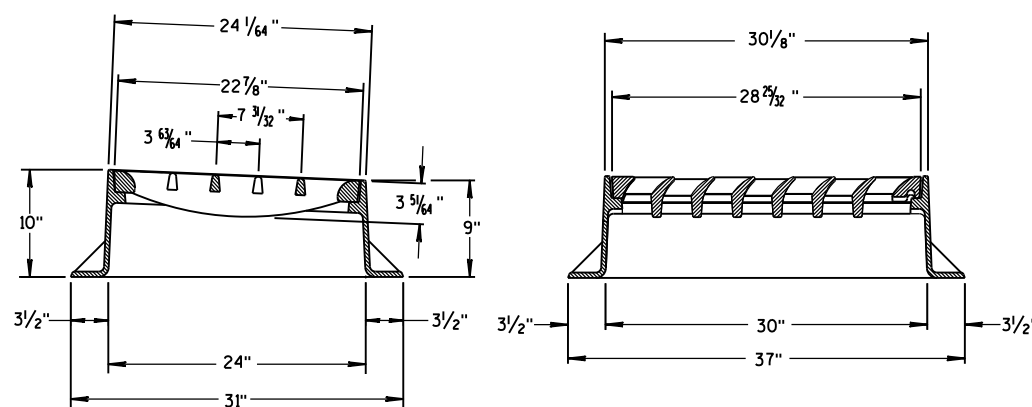
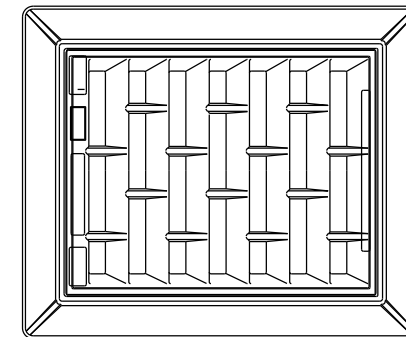
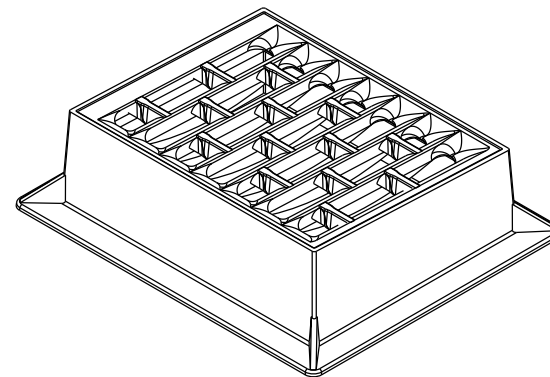
6



TYPE "L"



TYPE "M"



INLET COVER TYPE "BW"

6

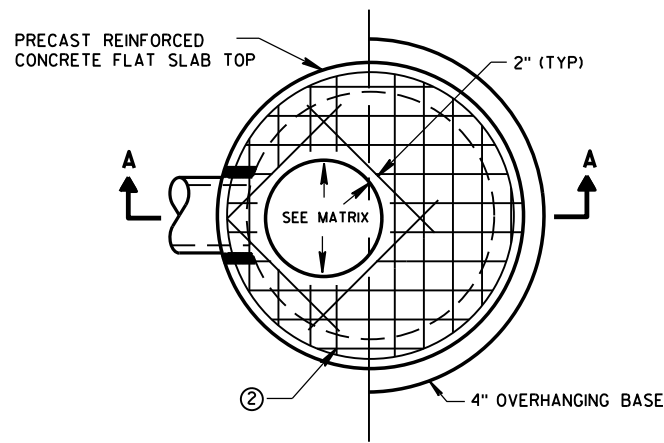
S.D.D. 8 A 5-19d

S.D.D. 8 A 5-19d

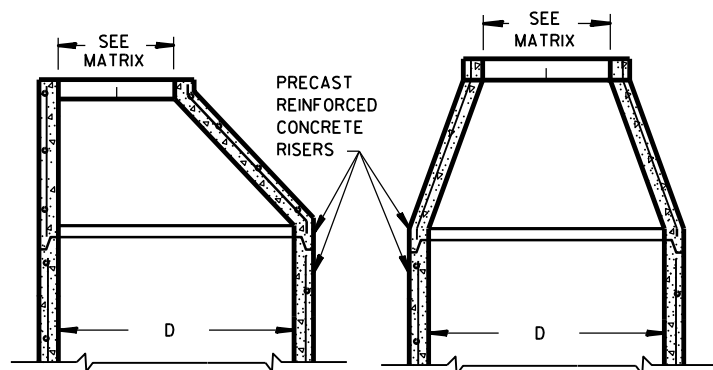
INLET COVER TYPE BW
MANHOLE COVERS, TYPE K,
J, J-S, L & M

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
11/27/2013 DATE /S/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA

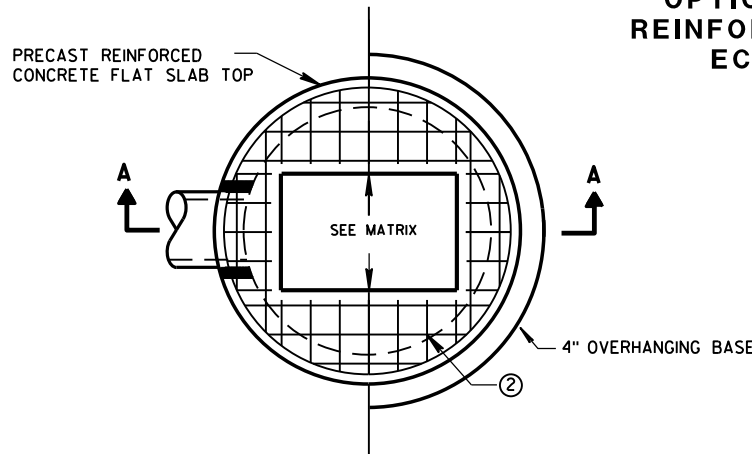


PLAN VIEW CIRCULAR OPENING

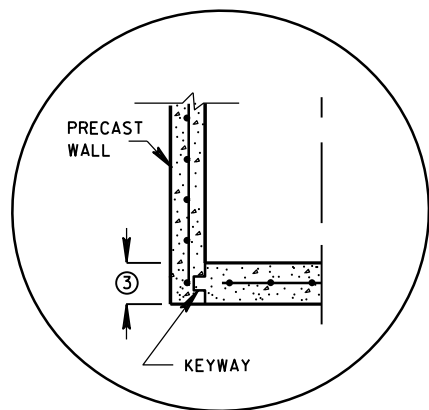


OPTIONAL PRECAST REINFORCED CONCRETE ECCENTRIC TOP

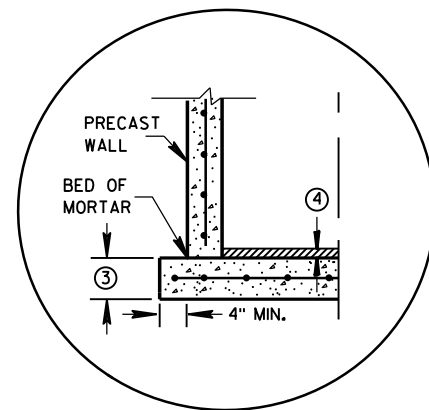
OPTIONAL PRECAST REINFORCED CONCRETE CONCENTRIC TOP



PLAN VIEW RECTANGULAR OPENING

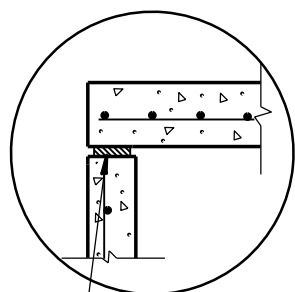


PRECAST REINFORCED CONCRETE WITH INTEGRAL BASE OPTION

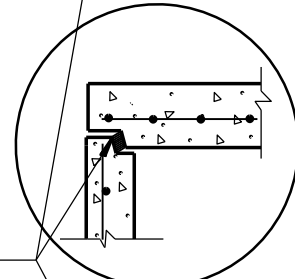


SEPARATE PRECAST REINFORCED CONCRETE BASE OPTION

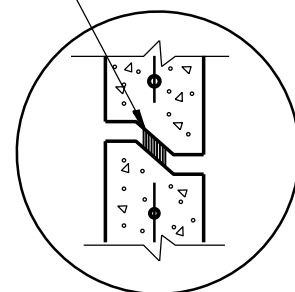
DETAIL "A"



TOP WITH PLAIN END JOINT

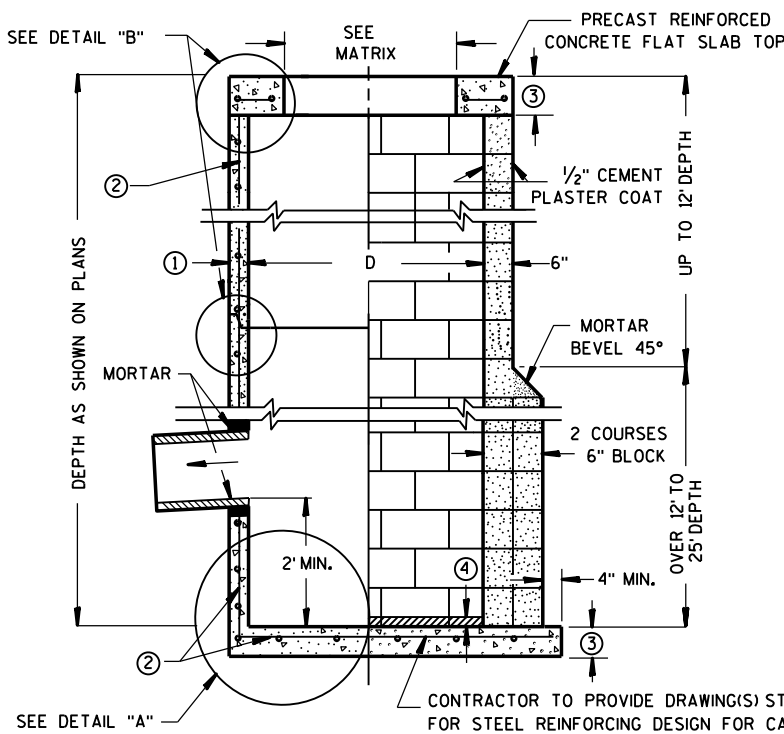


TOP WITH TONGUE AND GROOVE JOINT



RISER WITH TONGUE AND GROOVE JOINT

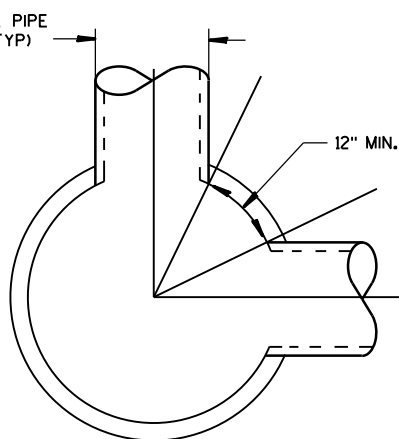
DETAIL "B"



SECTION A-A

PRECAST REINFORCED CONCRETE WITH MONOLITHIC BASE

CONCRETE BLOCK WITH CAST-IN-PLACE OR PRECAST REINFORCED CONCRETE BASE ②



DETAIL "C"

CATCH BASINS 3-FT, 4-FT, 5-FT AND 6-FT DIAMETER

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS. UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST CATCH BASIN UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL DRAINAGE STRUCTURES ARE DESIGNATED ON THE PLANS AS "MANHOLES 3X3-L", "CATCH BASINS 4-B", "INLETS 2X3-H", ETC. THE FIRST NUMBERS DESIGNATE THE SIZE OF THE STRUCTURE, AND THE FOLLOWING LETTER DESIGNATES THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF FOUNDATION BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

PRECAST REINFORCED CONCRETE CONE TOPS (ECCENTRIC OR CONCENTRIC) OR PRECAST REINFORCED CONCRETE FLAT SLAB TOPS MAY BE USED ON CONCRETE BLOCK STRUCTURES.

ECCENTRIC CONE TOPS MAY BE USED ON ALL STRUCTURES, AND CONCENTRIC CONE TOPS SHALL BE USED ONLY ON STRUCTURES 5 FEET OR LESS IN DEPTH, UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

STEPS MEETING AASHTO M199 AND THE FOLLOWING REQUIREMENTS SHALL BE INSTALLED IN ALL STRUCTURES OVER 5 FEET IN DEPTH; 16 INCH C-C MAXIMUM SPACING; PROJECT A MINIMUM CLEAR DISTANCE OF 4 INCHES FROM THE WALL AT THE POINT OF EMBEDMENT; MINIMUM LENGTH OF 10 INCHES; MINIMUM WALL EMBEDMENT OF 3 INCHES. FERROUS METAL STEPS NOT PAINTED OR TREATED TO RESIST CORROSION SHALL HAVE A MINIMUM CROSS SECTIONAL DIMENSION OF 1 INCH.

STEPS OF APPROVED POLYPROPYLENE PLASTIC COATED REINFORCEMENT BAR ARE ACCEPTABLE. REINFORCING BAR MUST BE A MINIMUM OF 1/2 INCH AND MEET THE REQUIREMENTS OF ASTM A615.

CERTIFICATION SHALL BE PROVIDED THAT INSTALLED STEPS WHEN TESTED IN ACCORDANCE WITH SECTION 10 OF AASHTO T280 CAN WITHSTAND A VERTICAL LOAD OF 800 LBS. AND A HORIZONTAL LOAD OF 400 LBS.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

ALL PRECAST INLET UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF AASHTO DESIGNATION M199.

PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

CONCRETE BLOCK WILL NOT BE PERMITTED FOR STRUCTURES GREATER THAN 4 FEET IN DIAMETER.

4" OVERHANGING BASES ARE REQUIRED FOR ALL CONCRETE BLOCK INSTALLATIONS. 4" OVERHANG IS REQUIRED WHEN SEPARATE PRECAST BASE IS PROVIDED. OVERHANG IS NOT REQUIRED ON PRECAST STRUCTURES WITH AN INTEGRAL OR MONOLITHIC BASE.

FOR ADDITIONAL CONFIGURATIONS, MAINTAIN A MINIMUM OF 12 INCHES AS MEASURED FROM THE INSIDE OF THE STRUCTURE WALL BETWEEN THE OUTSIDE PIPE WALLS OF ADJACENT PIPES. SEE DETAIL "C".

- ① MINIMUM WALL THICKNESS SHALL BE 4 INCHES FOR 3-FT, 5 INCHES FOR 4-FT, 6 INCHES FOR 5-FT AND 7 INCHES FOR 6-FT DIAMETER PRECAST CATCH BASINS.
- ② FOR PRECAST CATCH BASINS PROVIDE REINFORCING STEEL IN ACCORDANCE TO AASHTO M199.
- ③ PRECAST FLAT SLAB TOPS AND BASES WITH A DIAMETER OF 48" AND LESS SHALL HAVE A MINIMUM THICKNESS OF 6". PRECAST FLAT SLAB TOPS AND BASES WITH A DIAMETER LARGER THAN 48" SHALL HAVE A MINIMUM THICKNESS OF 8".
- ④ 1" CONCRETE KEY POURED AFTER INSTALLATION. 2" SUMP MEASURED FROM TOP OF KEY.

CATCH BASIN COVER OPENING MATRIX

CATCH BASIN SIZE	INLET COVER TYPE OPENING SIZE (FT)	ALL A'S	ALL B'S	BW	C	F	ALL H'S	S	T	V	WM	Z
3-FT	2X2	X	X					X		X		
	2 DIA.				X					X		X
4-FT- 6-FT	2X2	X	X					X		X		
	2X2.5			X				X	X	X	X	
	2 DIA.				X							X
	2X3						X					
	2.5X3											

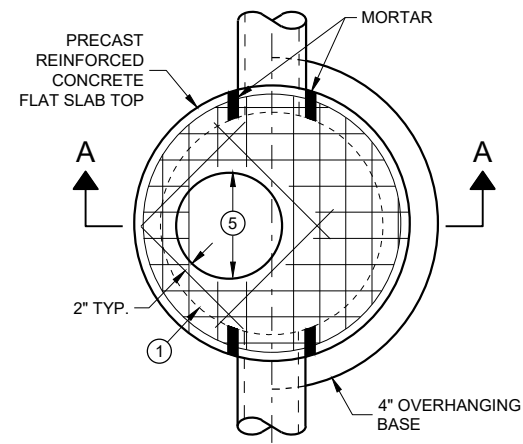
PIPE MATRIX

CATCH BASIN SIZE	MAXIMUM INSIDE PIPE DIAMETER FOR TWO PIPES	
	180° SEPARATION (IN)	90° SEPARATION (IN)
3-FT	15	12
4-FT	24	18
5-FT	36	24
6-FT	42	30

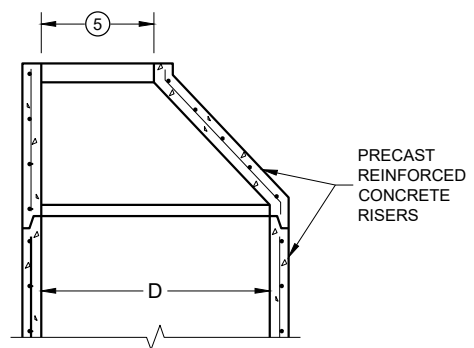
CATCH BASINS 3-FT, 4-FT, 5-FT AND 6-FT DIAMETER

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

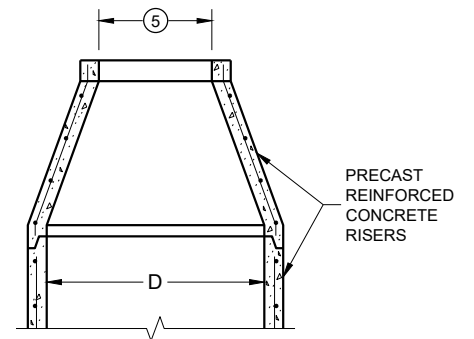
APPROVED
Sep 1, 2016 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
FHWA UNIT SUPERVISOR



**PLAN VIEW
CIRCULAR OPENING**



**OPTIONAL PRECAST
REINFORCED CONCRETE
ECCENTRIC TOP**



**OPTIONAL PRECAST
REINFORCED CONCRETE
CONCENTRIC TOP**

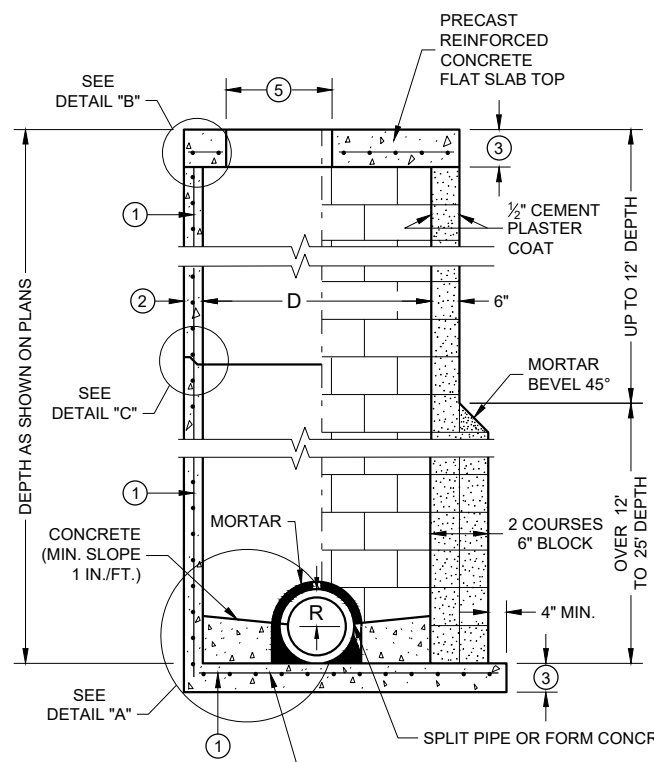
MANHOLE COVER OPENING MATRIX

MANHOLE COVER TYPE OPENING SIZE (FT.)	C	ALL J'S	K	L	M
2 DIA.	X	X		X	
3 DIA.			X		X

PIPE MATRIX

MANHOLE SIZE (DIA.)	MAXIMUM INSIDE PIPE DIAMETER FOR TWO PIPES		MINIMUM WALL THICKNESS (IN)	MINIMUM PRECAST FLAT SLAB TOP AND BASE THICKNESS
	180° SEPARATION (IN)	90° SEPARATION (IN)		
3-FT	15	12	4	6
4-FT	24	18	4	6
5-FT	36	24	5	8
6-FT	42	36	6	8
7-FT	48	36/42*	7	8
8-FT	60	42	8	8
9-FT	66	54	9	10
10-FT	72	60	10	10

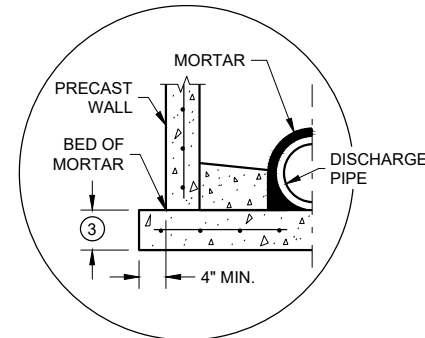
*A 36" PIPE AND A 42" PIPE CAN BE PLACED WITHIN 90 DEGREES. SEE MINIMUM HORIZONTAL PIPE SEPARATION DETAIL.



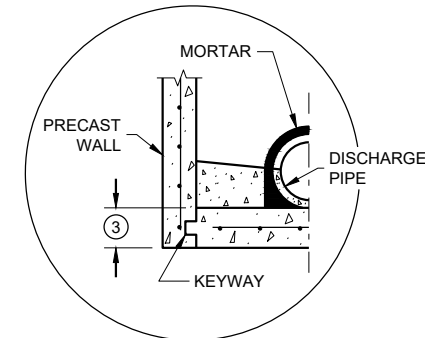
SECTION A - A

PRECAST REINFORCED CONCRETE WITH MONOLITHIC BASE

CONCRETE BLOCK WITH CAST IN PLACE OR PRECAST REINFORCED CONCRETE BASE ①

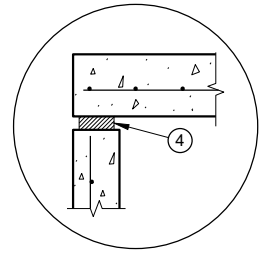


SEPARATE PRECAST REINFORCED CONCRETE BASE OPTION

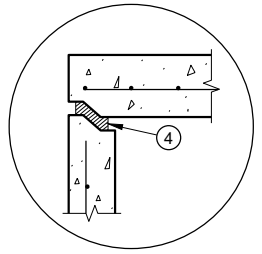


PRECAST REINFORCED CONCRETE WITH INTEGRAL BASE OPTION

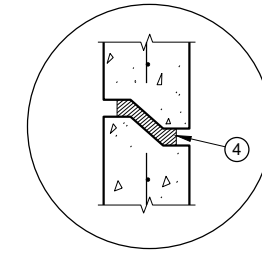
DETAIL "A"



TOP WITH PLAIN END JOINT



TOP WITH TONGUE AND GROOVE JOINT



RISER WITH TONGUE AND GROOVE JOINT

DETAIL "B"

DETAIL "C"

MANHOLES 3-FT, 4-FT, 5-FT, 6-FT, 7-FT, 8-FT, 9-FT AND 10-FT DIAMETER

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST MANHOLE UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL DRAINAGE STRUCTURES ARE DESIGNATED ON THE PLANS AS "MANHOLES 3X3-L", "CATCH BASINS 4-B", "INLETS 2X3-H", ETC. THE FIRST NUMBERS DESIGNATE THE SIZE OF THE STRUCTURE, AND THE FOLLOWING LETTER DESIGNATES THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF FOUNDATION BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

PRECAST REINFORCED CONCRETE CONE TOPS (ECCENTRIC OR CONCENTRIC) OR PRECAST REINFORCED CONCRETE FLAT SLAB TOPS MAY BE USED ON CONCRETE BLOCK STRUCTURES.

ECCENTRIC CONE TOPS MAY BE USED ON ALL STRUCTURES. CONCENTRIC CONE TOPS SHALL BE USED ONLY ON STRUCTURES 5 FEET OR LESS IN DEPTH UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

STEPS MEETING AASHTO M199 AND THE FOLLOWING REQUIREMENTS SHALL BE INSTALLED IN ALL STRUCTURES OVER 5 FEET IN DEPTH: 16 INCH C-C MAXIMUM SPACING; PROJECT A MINIMUM CLEAR DISTANCE OF 4 INCHES FROM THE WALL AT THE POINT OF EMBEDMENT; MINIMUM LENGTH OF 10 INCHES; MINIMUM WALL EMBEDMENT OF 3 INCHES. FERROUS METAL STEPS NOT PAINTED OR TREATED TO RESIST CORROSION SHALL HAVE A MINIMUM CROSS SECTIONAL DIMENSION OF 1 INCH.

STEPS OF APPROVED POLYPROPYLENE PLASTIC COATED REINFORCEMENT BAR ARE ACCEPTABLE. REINFORCING BAR MUST BE A MINIMUM OF 1/2 INCH AND MEET THE REQUIREMENTS OF ASTM A615.

CERTIFICATION SHALL BE PROVIDED THAT INSTALLED STEPS WHEN TESTED IN ACCORDANCE WITH SECTION 10 OF AASHTO T280 CAN WITHSTAND A VERTICAL LOAD OF 800 LBS. AND A HORIZONTAL LOAD OF 400 LBS.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

ALL PRECAST MANHOLE UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF AASHTO DESIGNATION M199.

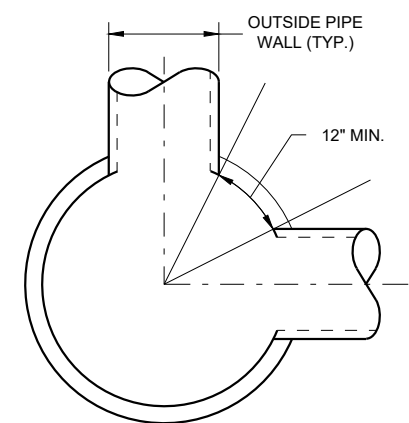
PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

CONCRETE BLOCK WILL NOT BE PERMITTED FOR STRUCTURES GREATER THAN 4 FEET IN DIAMETER.

4" OVERHANGING BASES ARE REQUIRED FOR ALL CONCRETE BLOCK INSTALLATIONS. 4" OVERHANG IS REQUIRED WHEN SEPARATE PRECAST BASE IS PROVIDED. OVERHANG IS NOT REQUIRED ON PRECAST STRUCTURES WITH AN INTEGRAL OR MONOLITHIC BASE.

FOR ADDITIONAL CONFIGURATIONS, MAINTAIN A MINIMUM OF 12 INCHES AS MEASURED FROM THE INSIDE OF THE STRUCTURE WALL BETWEEN THE OUTSIDE PIPE WALLS OF ADJACENT PIPES. SEE DETAIL "D".

- ① FOR PRECAST MANHOLES PROVIDE REINFORCING STEEL IN ACCORDANCE TO AASHTO M199.
- ② SEE PIPE MATRIX TABLE FOR MINIMUM WALL THICKNESS FOR PRECAST MANHOLES
- ③ SEE PIPE MATRIX TABLE FOR MINIMUM THICKNESS OF PRECAST FLAT SLAB TOPS AND BASES.
- ④ JOINTS TO BE SEALED WITH A BUTYL RUBBER SEAL PER SEALANT MANUFACTURERS RECOMMENDATIONS CONFORMING TO ASTM C 990 (TYP.).
- ⑤ SEE MANHOLE COVER OPENING MATRIX.

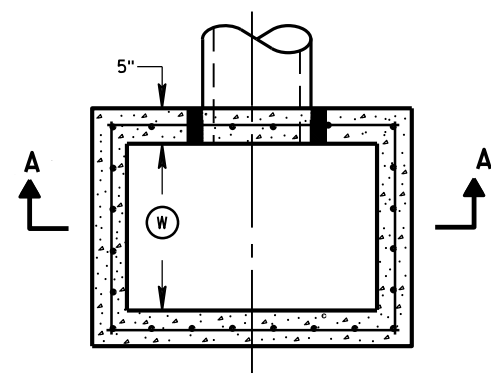


MINIMUM HORIZONTAL PIPE SEPARATION

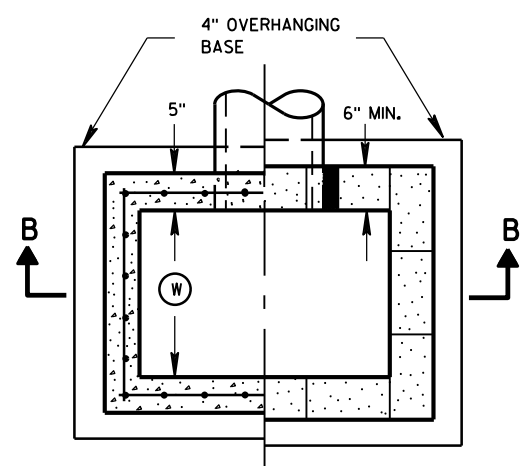
**MANHOLES, 3-FT, 4-FT
5-FT, 6-FT, 7-FT, 8-FT, 9-FT
AND 10-FT DIAMETER**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

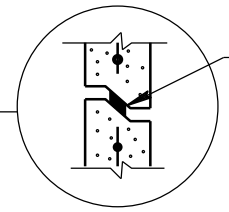
APPROVED
November 2021 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
ENGINEER
FHWA



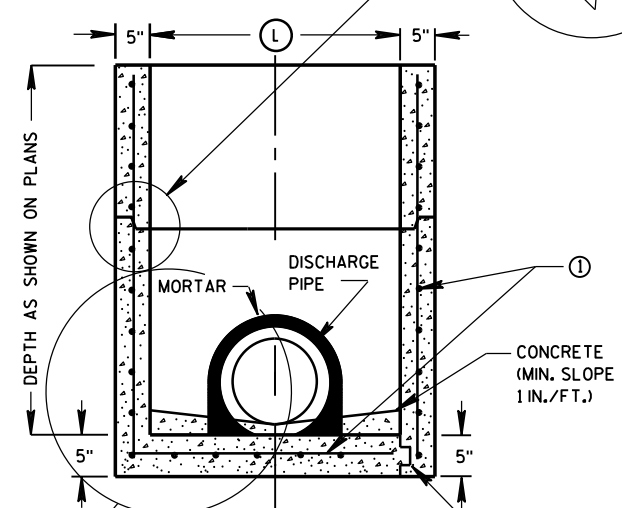
PLAN VIEW



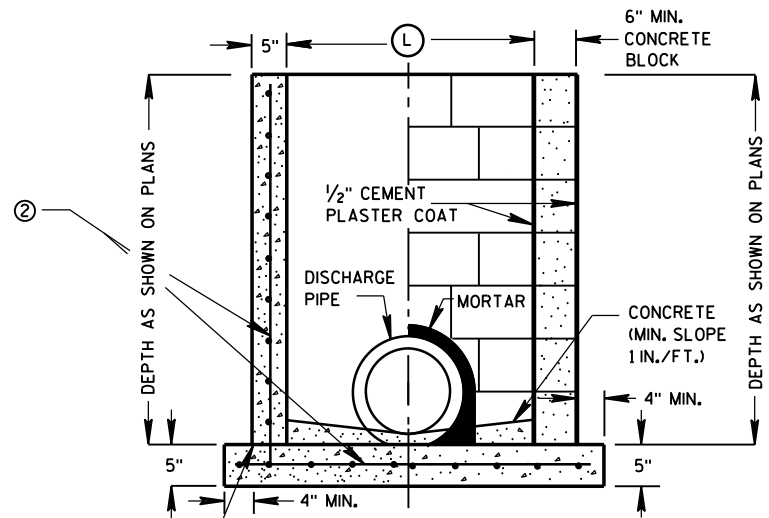
PLAN VIEW



RISER JOINTS TO BE SEALED WITH A BUTYL RUBBER SEAL PER SEALANT MANUFACTURERS RECOMMENDATIONS CONFORMING TO ASTM C 990 (TYP)



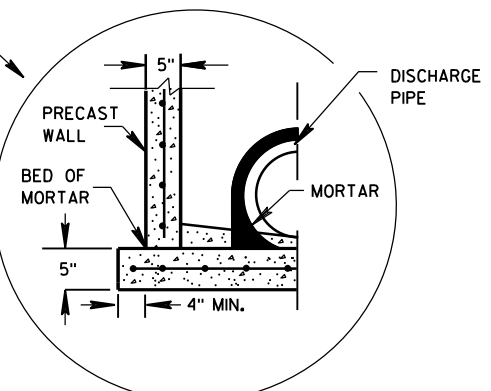
SECTION A-A



SECTION B-B

PRECAST REINFORCED CONCRETE WITH MONOLITHIC BASE
 PRECAST REINFORCED CONCRETE WITH INTEGRAL BASE
 KEYWAY

CAST-IN-PLACE REINFORCED CONCRETE
 CONCRETE BLOCK WITH CAST-IN-PLACE OR PRECAST REINFORCED CONCRETE BASE ①



SEPARATE PRECAST REINFORCED CONCRETE BASE OPTION

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST INLET UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL PRECAST INLET UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF ASTM C 913.

ALL DRAINAGE STRUCTURES ARE DESIGNATED ON THE PLANS AS "MANHOLES 3X3-L", "CATCH BASINS 4-B", "INLETS 2X3-H", ETC. THE FIRST NUMBERS DESIGNATES THE SIZE OF THE STRUCTURE, AND THE FOLLOWING LETTER DESIGNATES THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF FOUNDATION BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

4" OVERHANGING BASES ARE REQUIRED FOR CAST-IN-PLACE REINFORCED CONCRETE AND CONCRETE BLOCK INSTALLATIONS. 4" OVERHANG IS REQUIRED WHEN SEPARATE PRECAST BASE IS PROVIDED. OVERHANG IS NOT REQUIRED ON PRECAST STRUCTURES WITH AN INTEGRAL OR MONOLITHIC BASE.

MAXIMUM INSIDE PIPE DIAMETER DETERMINED BY 3 INCH CLEARANCE ON EACH SIDE OF THE OUTSIDE WALL OF THE PIPE. SEE DETAIL "A". ASSUMES PIPE ENTERS PERPENDICULAR TO THE STRUCTURE.

① FOR PRECAST INLETS PROVIDE REINFORCING STEEL IN ACCORDANCE TO ASTM C 913.

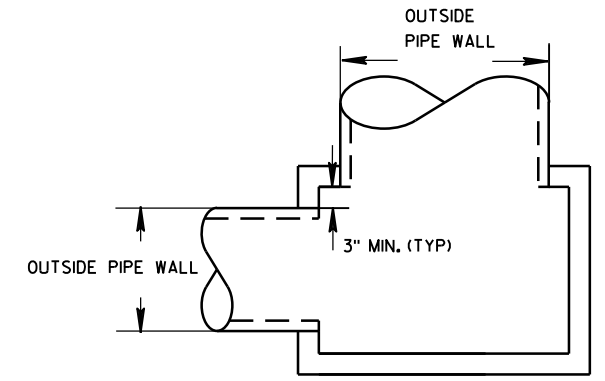
② CONTRACTOR TO PROVIDE DRAWING(S) STAMPED BY A PROFESSIONAL ENGINEER FOR STEEL REINFORCING DESIGN FOR CAST-IN-PLACE STRUCTURES.

INLET COVER MATRIX

INLET SIZE	INLET COVER TYPE		ALL A'S	ALL B'S	BW	F	ALL H'S	S	T	V	WM
	WIDTH (W) (FT)	LENGTH (L) (FT)									
2X2-FT	2	2	X	X				X		X	
2X2.5-FT	2	2.5			X			X	X	X	X
2X3-FT	2	3					X				
2.5X3-FT	2.5	3				X					

PIPE MATRIX

INLET SIZE	MAXIMUM INSIDE PIPE DIAMETER	
	WIDTH (IN)	LENGTH (IN)
2X2-FT	12	12
2X2.5-FT	12	18
2X3-FT	12	24
2.5X3-FT	18	24



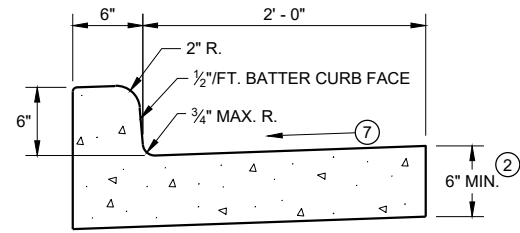
DETAIL "A"

INLETS 2X2-FT, 2X2.5-FT, 2X3-FT AND 2.5X3-FT

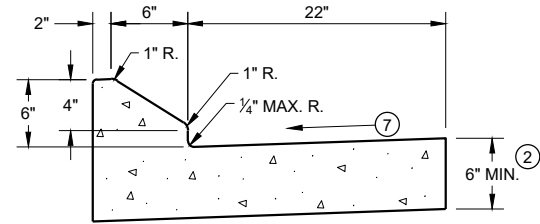
INLETS 2X2-FT, 2X2.5-FT, 2X3-FT AND 2.5X3-FT

STATE OF WISCONSIN
 DEPARTMENT OF TRANSPORTATION

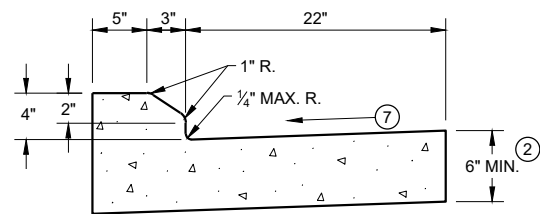
APPROVED
 Sept., 2016 /S/ Rodney Taylor
 DATE ROADWAY STANDARDS DEVELOPMENT
 FHWA UNIT SUPERVISOR



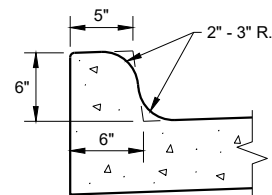
TYPES A¹ & D



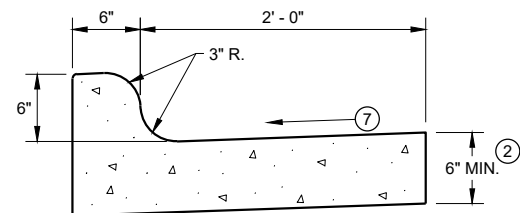
6" SLOPED CURB TYPES G¹ & J



4" SLOPED CURB TYPES G¹ & J

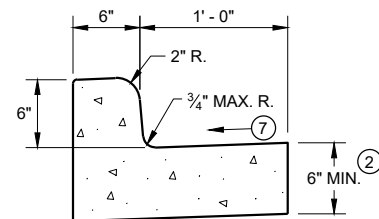


TYPES K¹ & L
(OPTIONAL CURB SHAPE)



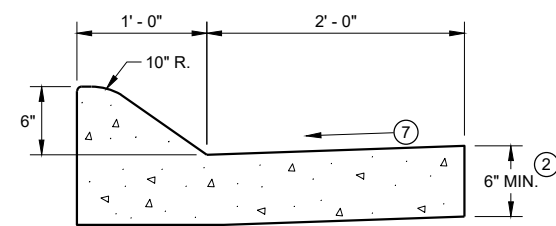
TYPES K¹ & L

CONCRETE CURB AND GUTTER 30"

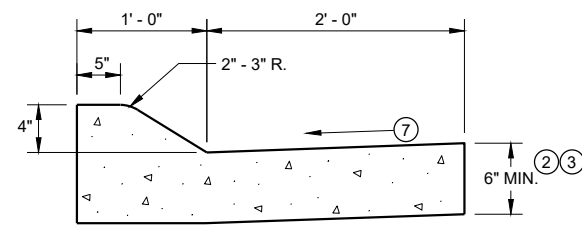


TYPES A¹ & D

CONCRETE CURB AND GUTTER 18"

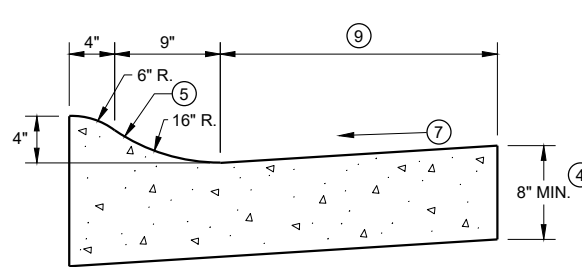


6" SLOPED CURB TYPES A¹ & D



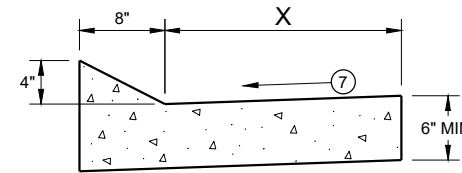
4" SLOPED CURB TYPES A¹ & D

CONCRETE CURB AND GUTTER 36"



4" SLOPED CURB TYPES R¹ & T

TBT & TBTT	X
30"	22"
36"	28"

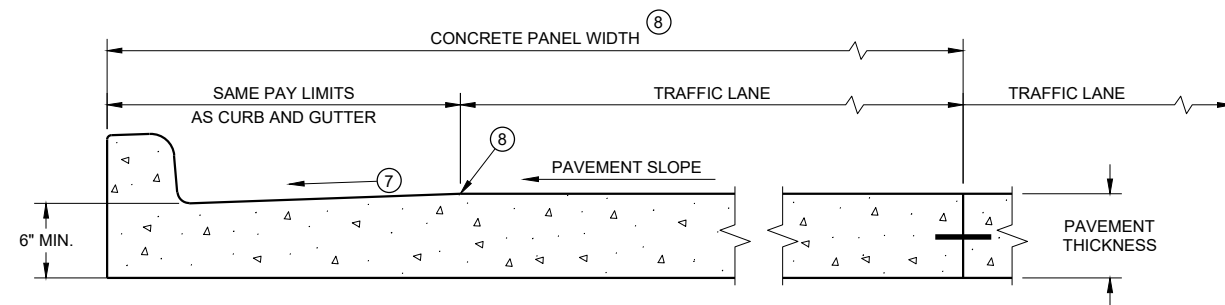


TYPES TBT & TBTT¹

CONCRETE CURB AND GUTTER

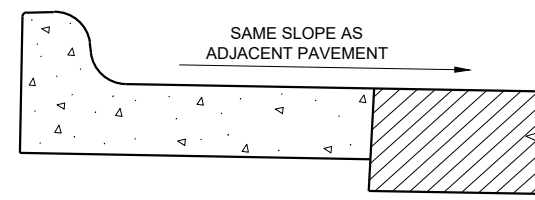
PAVEMENT THICKNESS AND MAXIMUM CONCRETE PANEL WIDTH TABLE

PAVEMENT THICKNESS	MAXIMUM PANEL WIDTH
LESS THAN 10"	12'
10" & ABOVE	15'



PARTIAL SECTION OF PAVEMENT *
WITH INTEGRAL CURB AND GUTTER

* BIKE LANE IS NOT SHOWN



REVERSE SLOPE GUTTER⁶
(TYPICAL FOR ALL CURB & GUTTER TYPES)

GENERAL NOTES

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

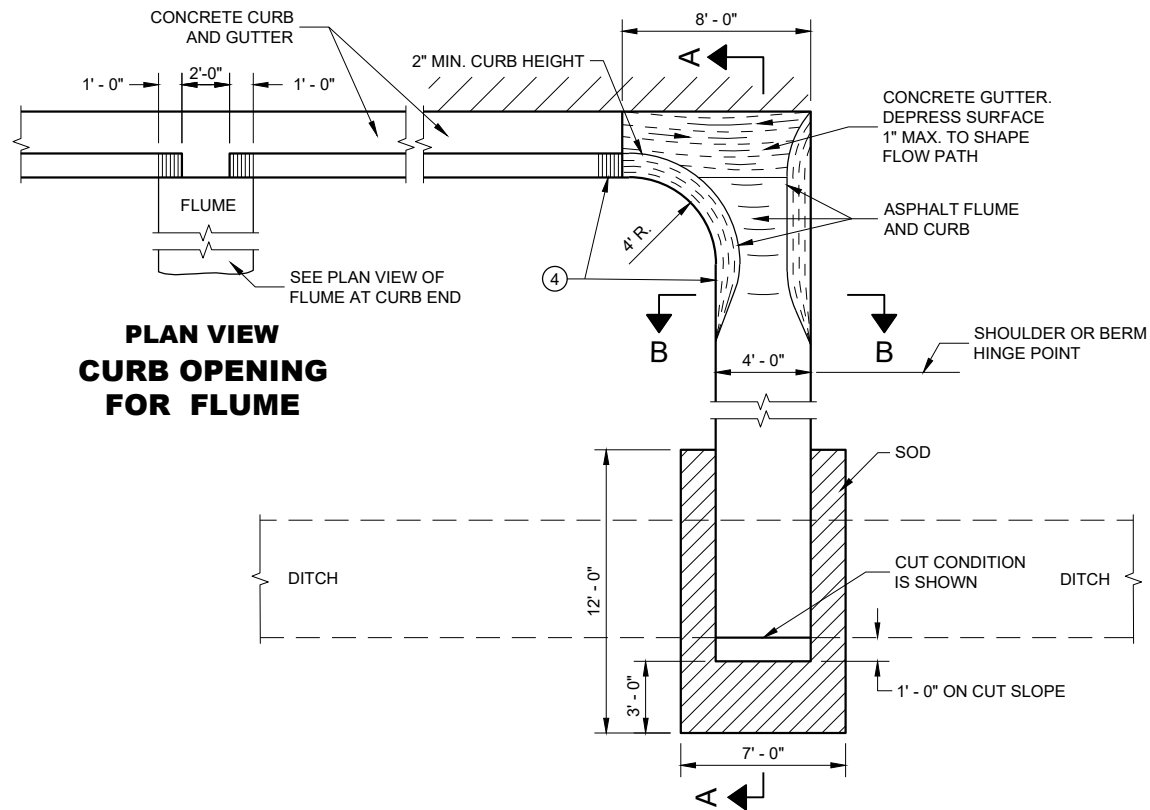
INTEGRAL CURB AND GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB AND GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ③ USE 8" MINIMUM GUTTER THICKNESS WHEN USED WITH AN ADJACENT CONCRETE TRUCK APRON PLACED BEHIND BACK OF CURB.
- ④ THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑤ UNLESS OTHERWISE NOTED, FOR STAKING PURPOSES THE FACE OF CURB IS 6" FROM THE BACK OF CURB.
- ⑥ WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.
- ⑦ USE 4% GUTTER CROSS SLOPE UNLESS OTHERWISE NOTED IN THE PLANS.
- ⑧ INCLUDE LONGITUDINAL JOINT AND TIE BARS ALONG LANE EDGE WHEN CONCRETE PANEL WIDTH EXCEEDS THE MAXIMUM WIDTH PER TABLE BELOW. LONGITUDINAL JOINT(S) ARE NOT ALLOWED WITHIN TRAFFIC LANES AND BIKE LANES. LONGITUDINAL JOINT MAY BE SAWED.
- ⑨ CONCRETE CURB AND GUTTER 4-INCH SLOPED 30-INCH TYPE "R" AND "T" = 17 INCHES
CONCRETE CURB AND GUTTER 4-INCH SLOPED 36-INCH TYPE "R" AND "T" = 23 INCHES

NOTE: TAPER CURB ENDS TO GUTTER IN 1' - 0"

ASPHALTIC FLUME



**PLAN VIEW
CURB OPENING
FOR FLUME**

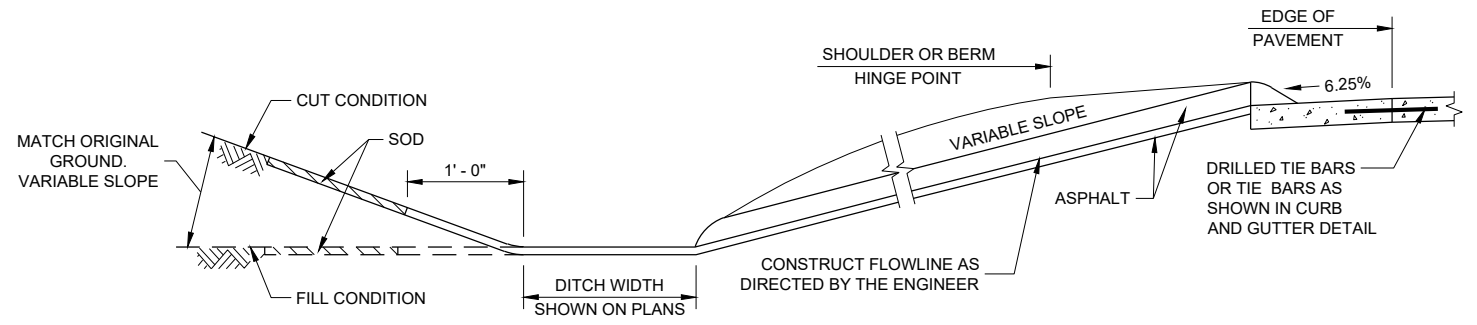
**PLAN VIEW
FLUME AT CURB END**

GENERAL NOTES

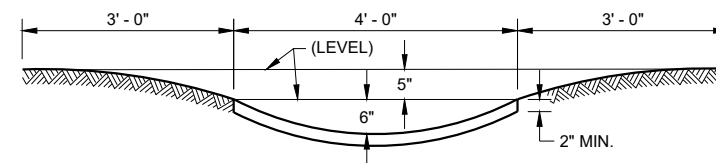
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

4" X 4" - W3.0 X W3.0 CONCRETE REINFORCEMENT SHALL BE IN ACCORDANCE WITH AASHTO SPECIFICATION M55.

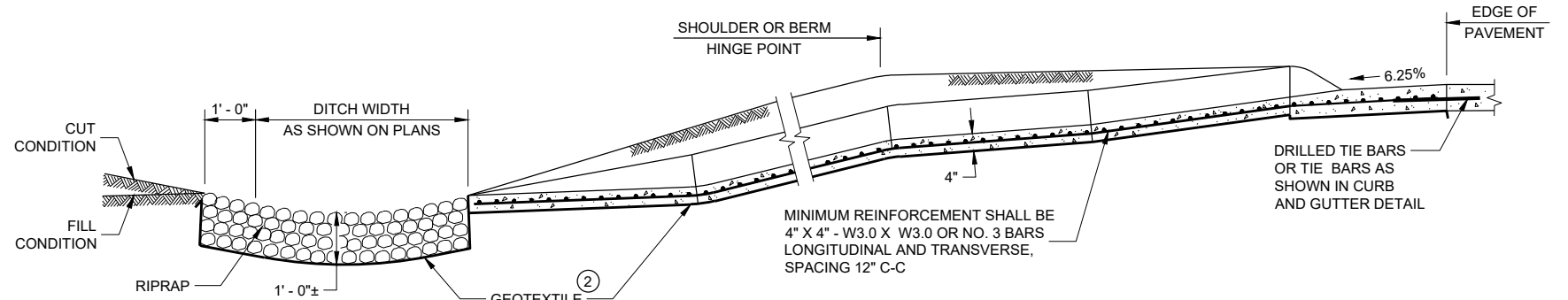
- ① JOINTS SHALL BE 1/8" TO 1/4" WIDE BY 1 1/2" DEEP AND SPACED AT UNIFORM INTERVALS OF APPROXIMATELY 4 FEET.
- ② GEOTEXTILE TYPE "R" SHALL UNDERLAY THE FULL LENGTH AND WIDTH OF THE CONCRETE SURFACE DRAIN AND RIPRAP.
- ③ CONCRETE SURFACE DRAIN WITHOUT CURB AND GUTTER MAY BE USED ON BACKSLOPES WHEN SPECIFIED.
- ④ ANGLE OF FLUME IN RELATION TO BACK OF CURB TO BE CONSTRUCTED PER THE PLAN DETAILS OR AS DIRECTED BY THE ENGINEER. ANGLE OF FLUME MAY BE OTHER THAN 90 DEGREES AS SHOWN.



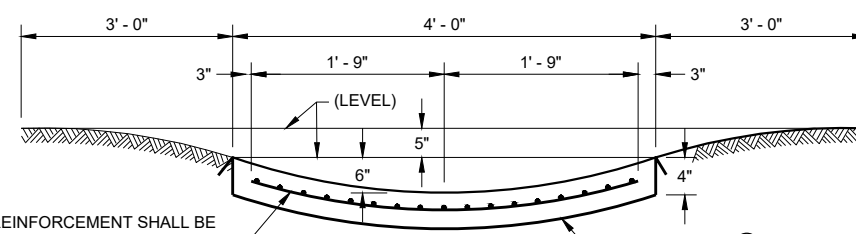
SECTION A - A



SECTION B - B

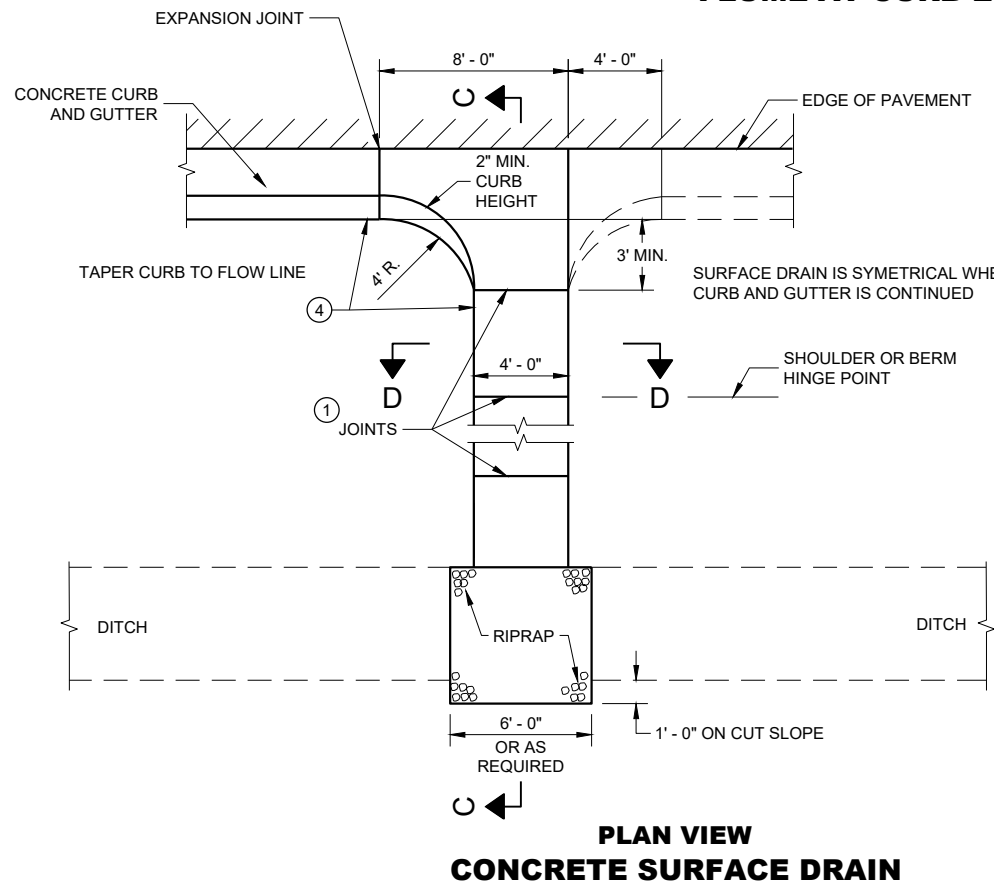


SECTION C - C



SECTION D - D

MINIMUM REINFORCEMENT SHALL BE 4" X 4" - W3.0 X W3.0 OR NO. 3 BARS LONGITUDINAL AND TRANSVERSE, SPACING 12" C-C



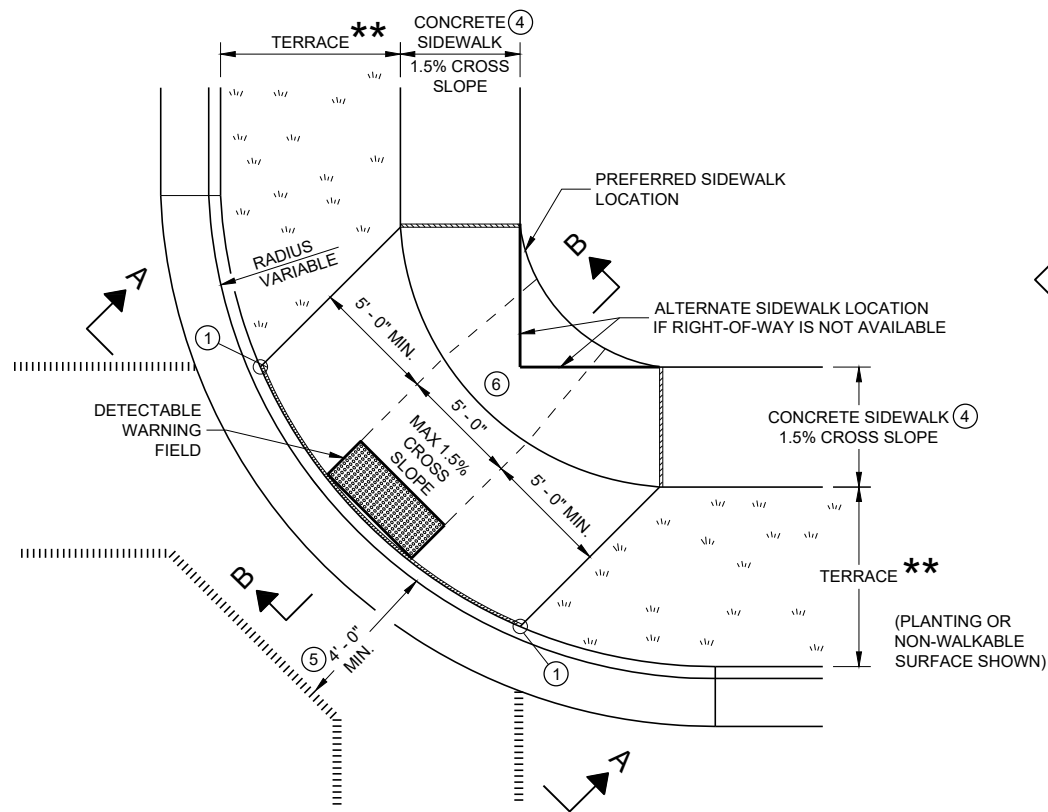
**PLAN VIEW
CONCRETE SURFACE DRAIN**

CONCRETE SURFACE DRAINS AND ASPHALTIC FLUMES

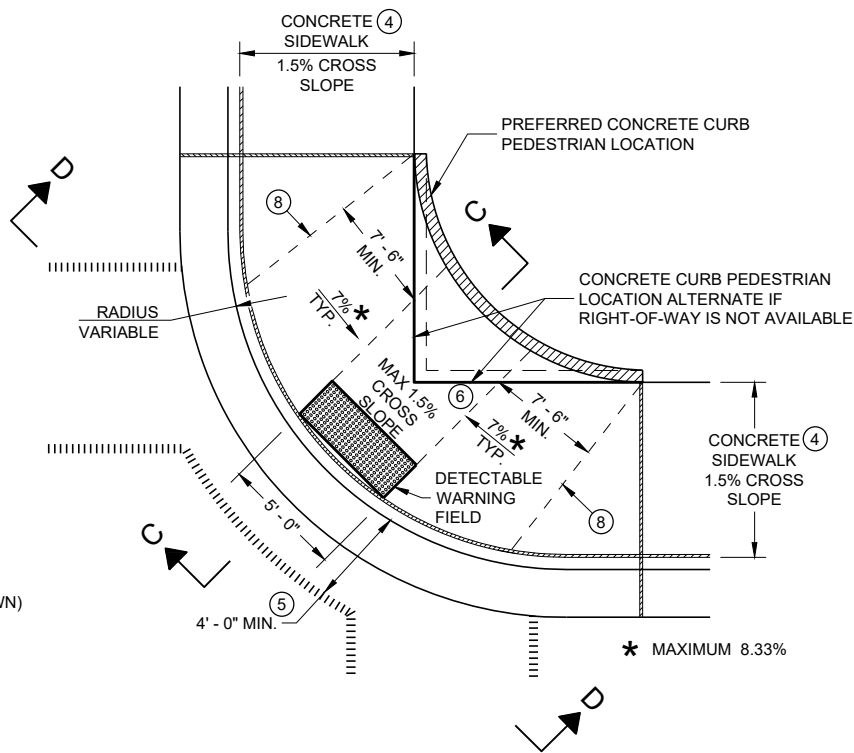
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2021 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
ENGINEER

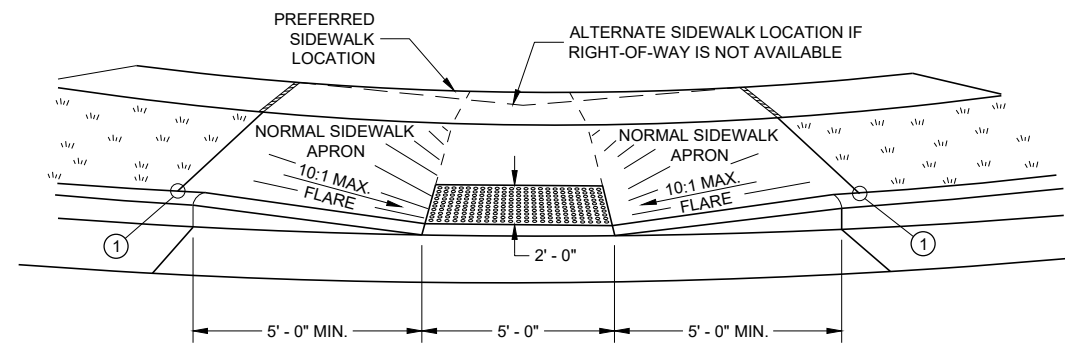
FHWA



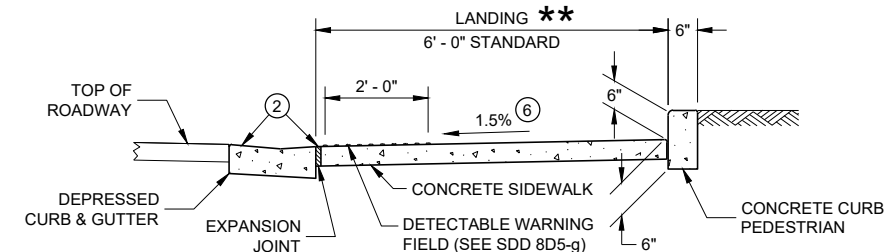
**PLAN VIEW
CURB RAMP TYPE 1
(CENTER OF CORNER RADIUS)**



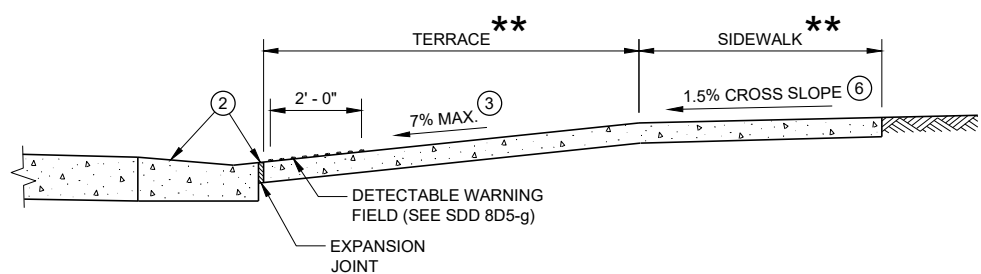
**PLAN VIEW
CURB RAMP TYPE 1 - A
(NO TERRACE)**



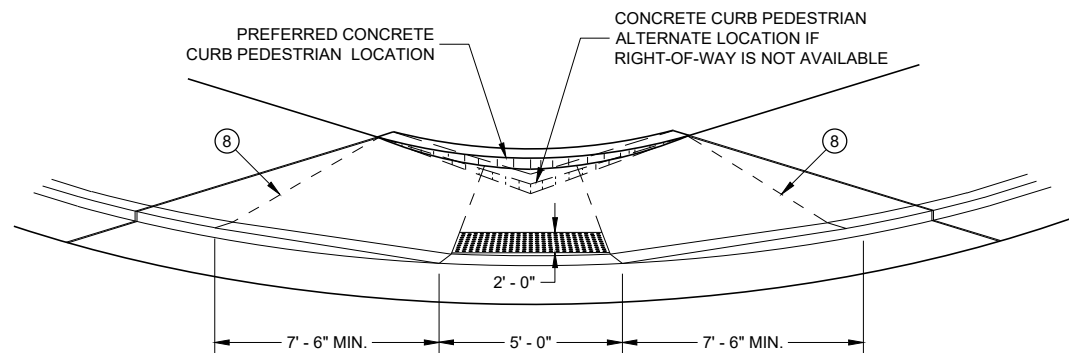
VIEW A - A FOR TYPE 1



SECTION C - C FOR TYPE 1 - A



SECTION B - B FOR TYPE 1



VIEW D - D FOR TYPE 1 - A

GENERAL NOTES

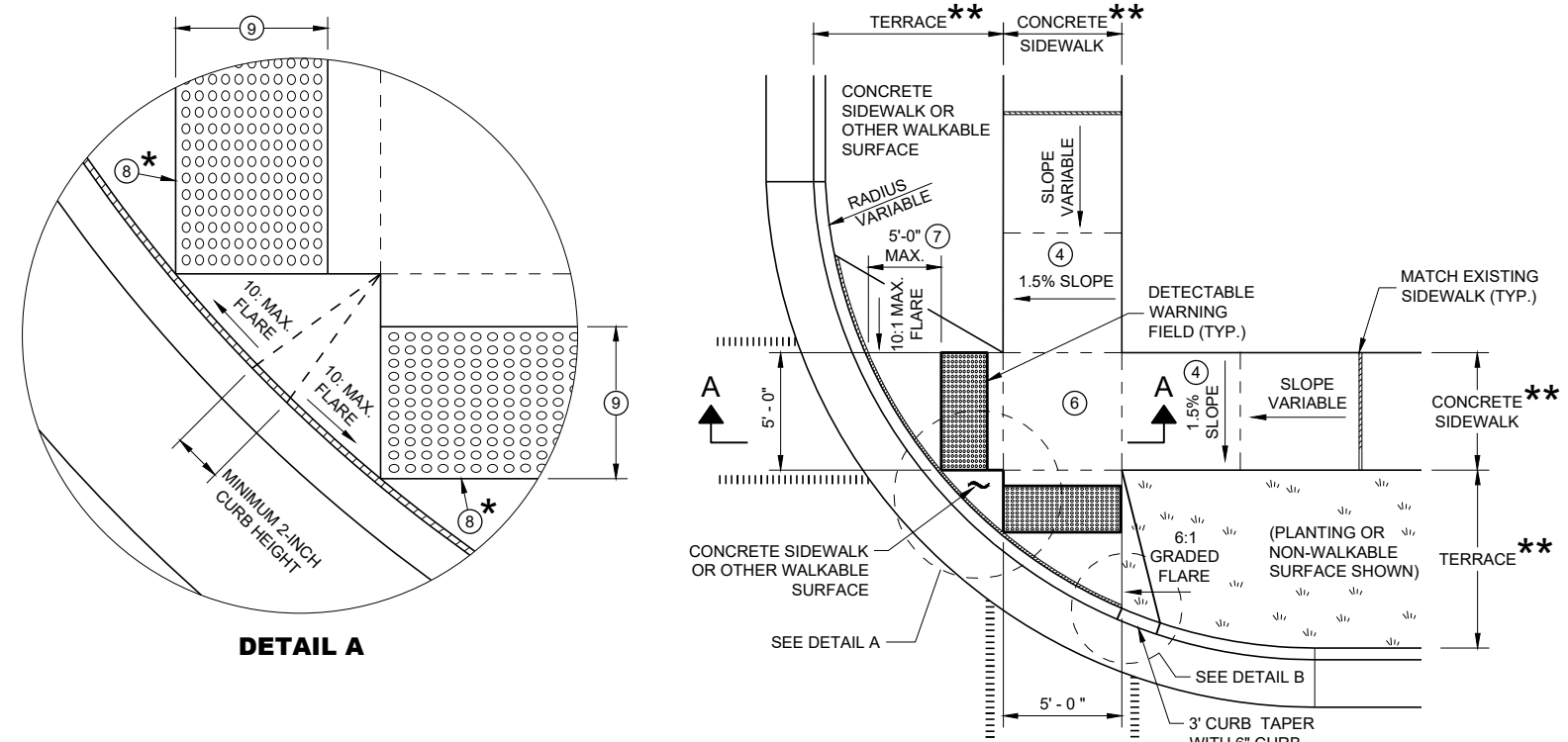
- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- WHEN NECESSARY, THE SIDEWALK ELEVATION MAY BE LOWERED TO MEET THE HIGH POINT ON THE RAMP.
- TYPE 1 CURB RAMPS SHALL HAVE A NORMAL SIDEWALK APRON AND CURB ON BOTH SIDES OF RAMP.
- DETECTABLE WARNING FIELD SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS "CURB RAMP DETECTABLE WARNING FIELD". THE CONCRETE PEDESTRIAN CURB, IF NEEDED, SHALL BE MEASURED AND PAID BY THE LINEAR FOOT AS "CONCRETE CURB PEDESTRIAN". CONCRETE SIDEWALK IN THE CURB RAMP AREA SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS CONCRETE SIDEWALK, INCLUDING THE AREA UNDER THE DETECTABLE WARNING FIELD.
- SELECT CURB RAMP DETECTABLE WARNING FIELD MATERIALS AND DEVICES FROM THE DEPARTMENT'S APPROVED MATERIALS LIST. THE COLOR OF THE DETECTABLE WARNING FIELD IS SPECIFIED ELSEWHERE AND IS INCIDENTAL TO THE BID ITEM OF "CURB RAMP DETECTABLE WARNING FIELD"
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- SURFACE TEXTURE OF THE RAMP SHALL BE OBTAINED BY COARSE BROOMING TRANSVERSE TO THE SLOPE OF THE RAMP.
- ① THIS POINT IS AN EXTENSION OF OUTSIDE EDGE OF APPROACHING SIDEWALK WHERE IT MEETS THE BACK OF CONCRETE CURB. POINT LOCATION MAY BE ADJUSTED TO ALIGN WITH BEGINNING OF FULL-HEIGHT CURB IF THIS DISTANCE IS SHORT.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4" INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ MAXIMUM 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑤ PROVIDE A LEVEL LANDING IN THE STREET AND GUTTER AREA (2% MAXIMUM SLOPE IN ANY DIRECTION). WHEN THE GUTTER SLOPE EXCEEDS 2%, CONSTRUCT THE LEVEL LANDING IN THE STREET AREA.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

LEGEND

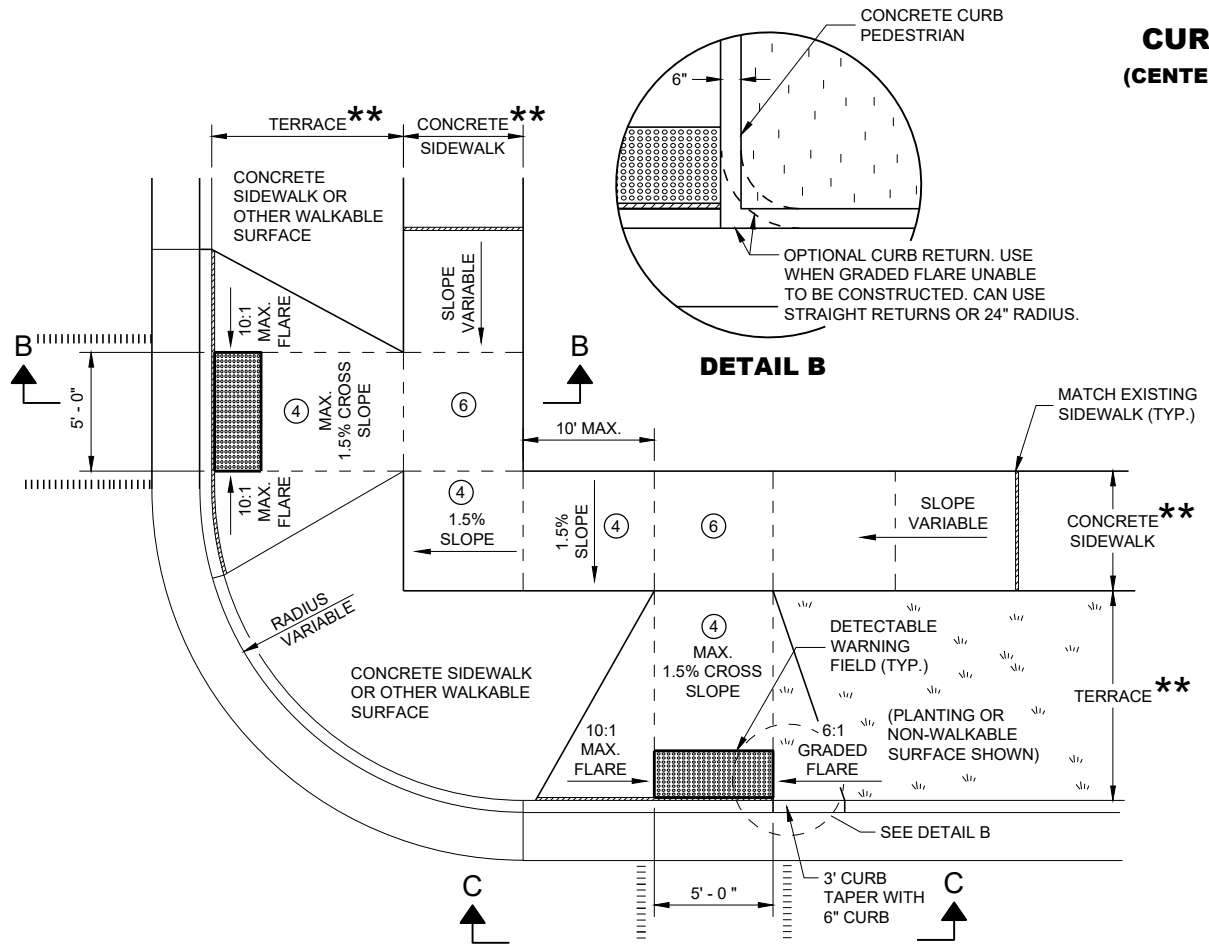
- 1/2" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)

**CURB RAMPS
TYPE 1 AND 1-A**

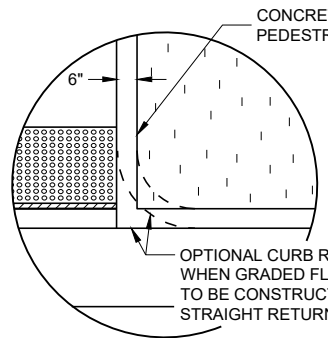
STATE OF WISCONSIN
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PLAN VIEW CURB RAMP TYPE 2 (CENTER OF CORNER RADIUS)



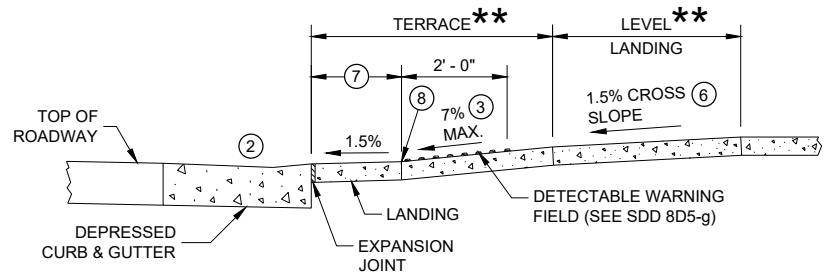
PLAN VIEW CURB RAMP TYPE 3 (OUTSIDE OF CROSSWALK AREA)



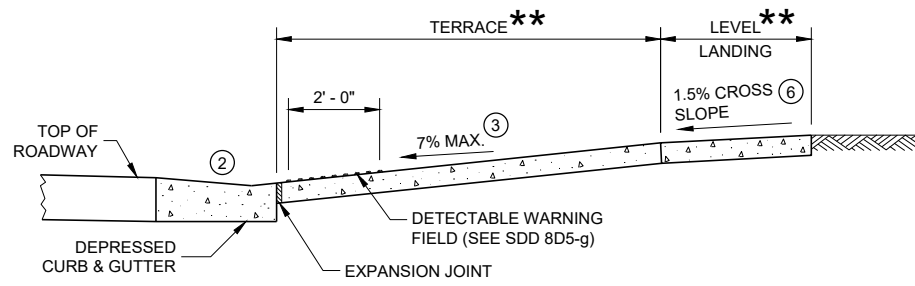
DETAIL B

GENERAL NOTES

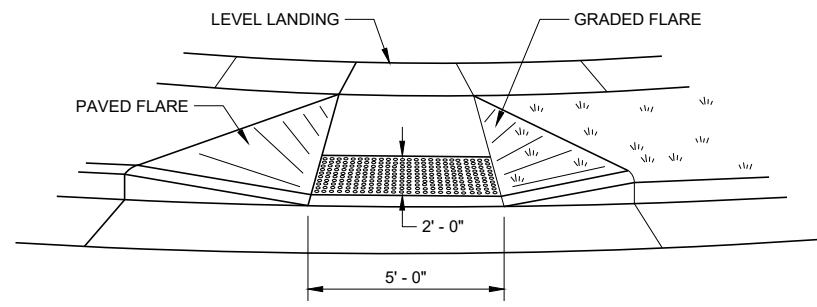
- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE (2.67% OR LESS) AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET X 5 FEET.
- ⑦ WHEN GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑨ WHEN DISTANCE IS LESS THAN 6' - 0", IT MAY BE DIFFICULT TO ACHIEVE A 7% SLOPE OR FLATTER ALONG THE RAMP. REDUCE CURB HEIGHT IN TRIANGLE AREA TO ACHIEVE 7% SLOPE OR FLATTER ON RAMP. CONSTRUCT 2-INCH MINIMUM CURB HEIGHT BETWEEN 10:1 FLARES.



SECTION A - A FOR TYPE 2



SECTION B - B FOR TYPE 3



VIEW C - C FOR TYPE 3

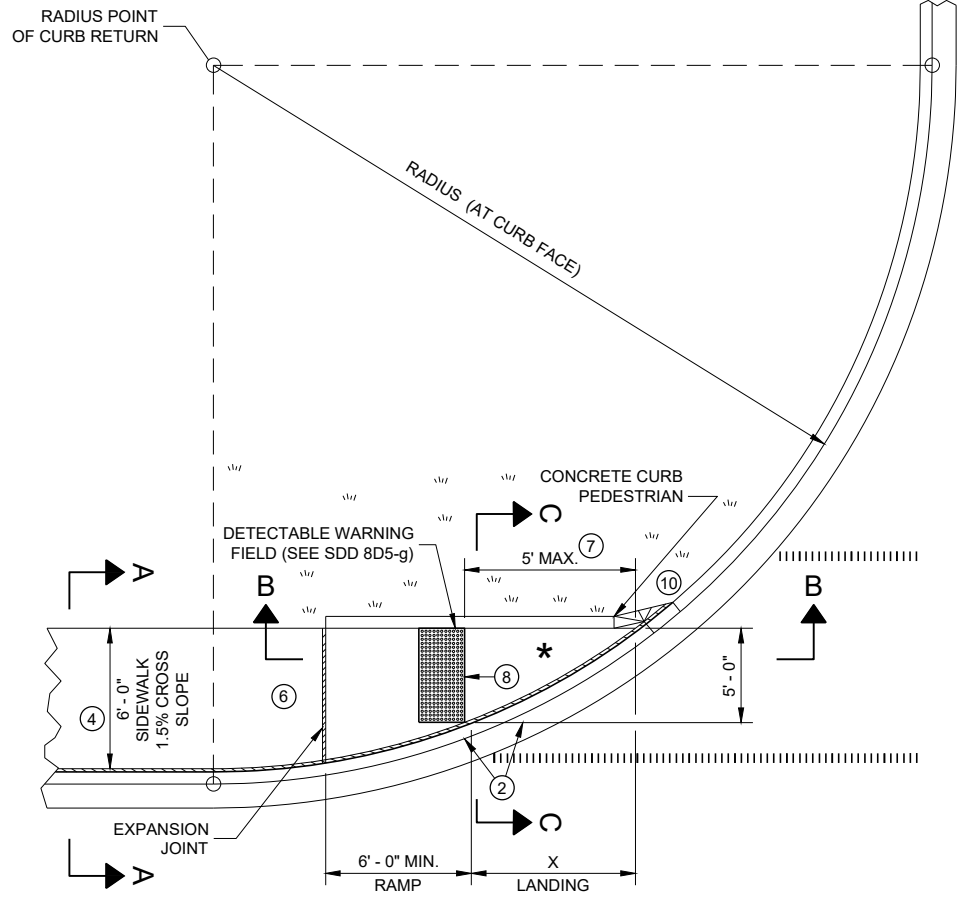
- * MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK
- ** WIDTH SHOWN ELSEWHERE IN THE PLANS

LEGEND

- 1/2" EXPANSION JOINT SIDEWALK
- - - - - CONTRACTION JOINT SIDEWALK
- ||||| PAVEMENT MARKING CROSSWALK (WHITE)

CURB RAMPS TYPE 2 AND 3

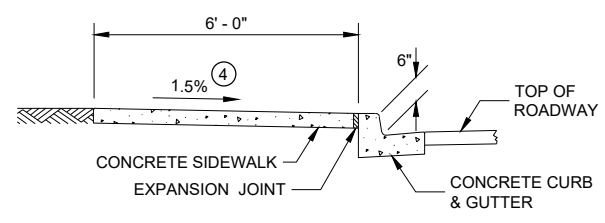
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**PLAN VIEW
CURB RAMP TYPE 4A**

RADIUS (AT CURB FACE)	X
10 FEET	4' - 7"
15 FEET	6' - 5 1/2"

INTERMEDIATE RADII CAN BE INTERPOLATED



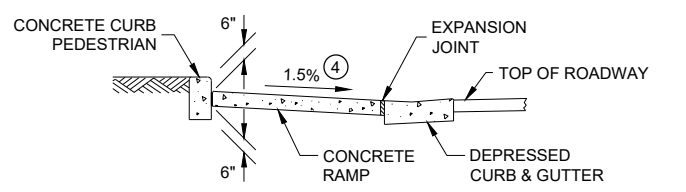
SECTION A - A FOR TYPE 4A

GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4" INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑦ WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑩ INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

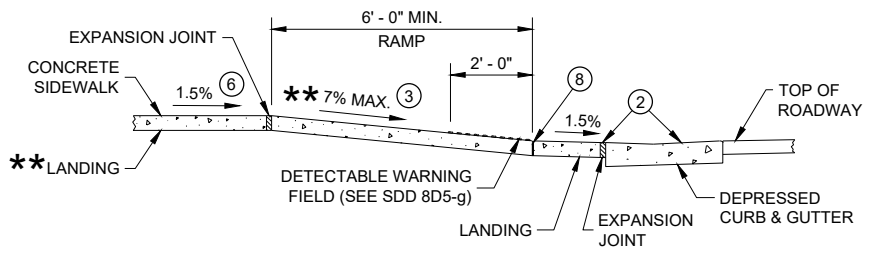
LEGEND

- 1/2" EXPANSION JOINT SIDEWALK
- - - CONTRACTION JOINT SIDEWALK
- ||||| PAVEMENT MARKING CROSSWALK (WHITE)



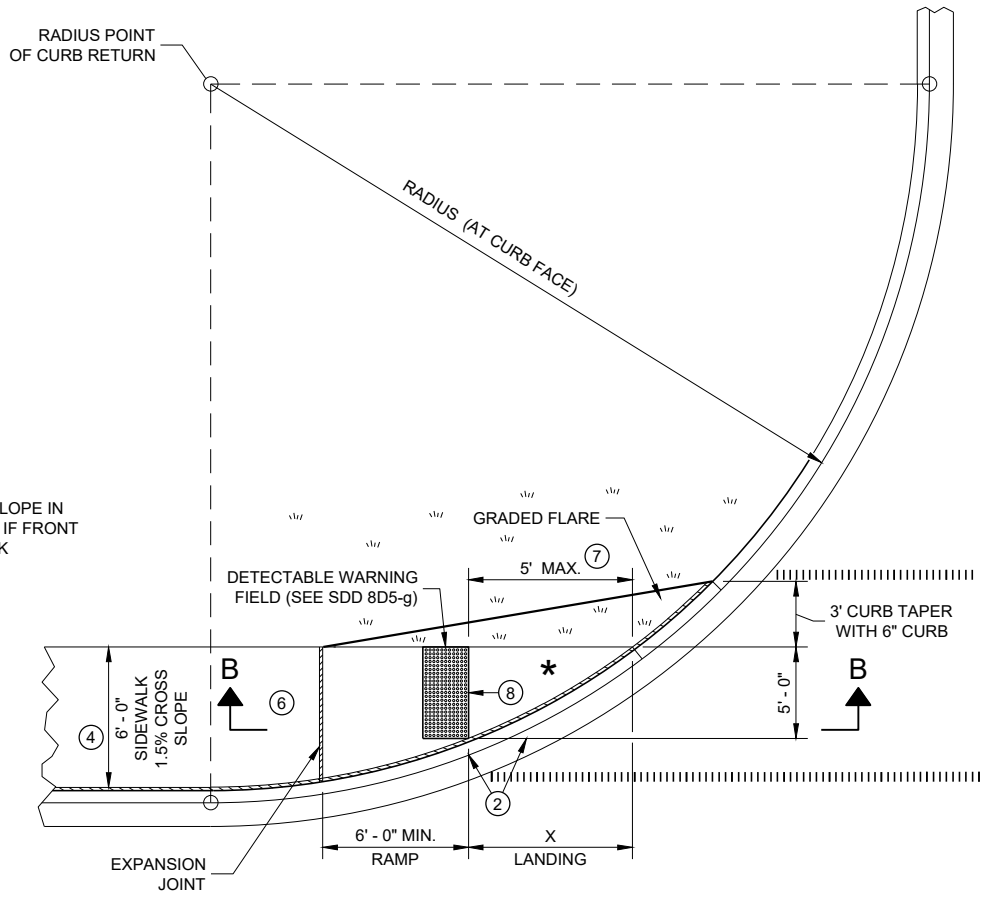
SECTION C - C FOR TYPE 4A

* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IF FRONT OF GRADE BREAK

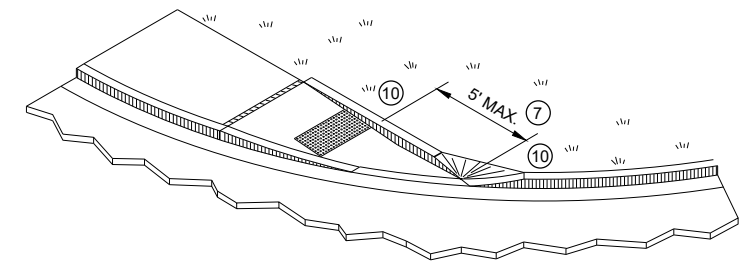


**SECTION B - B FOR
TYPE 4A AND TYPE 4A1**

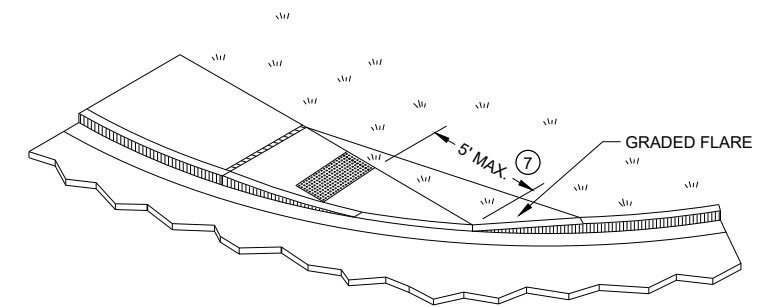
** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED



**PLAN VIEW
CURB RAMP TYPE 4A1**



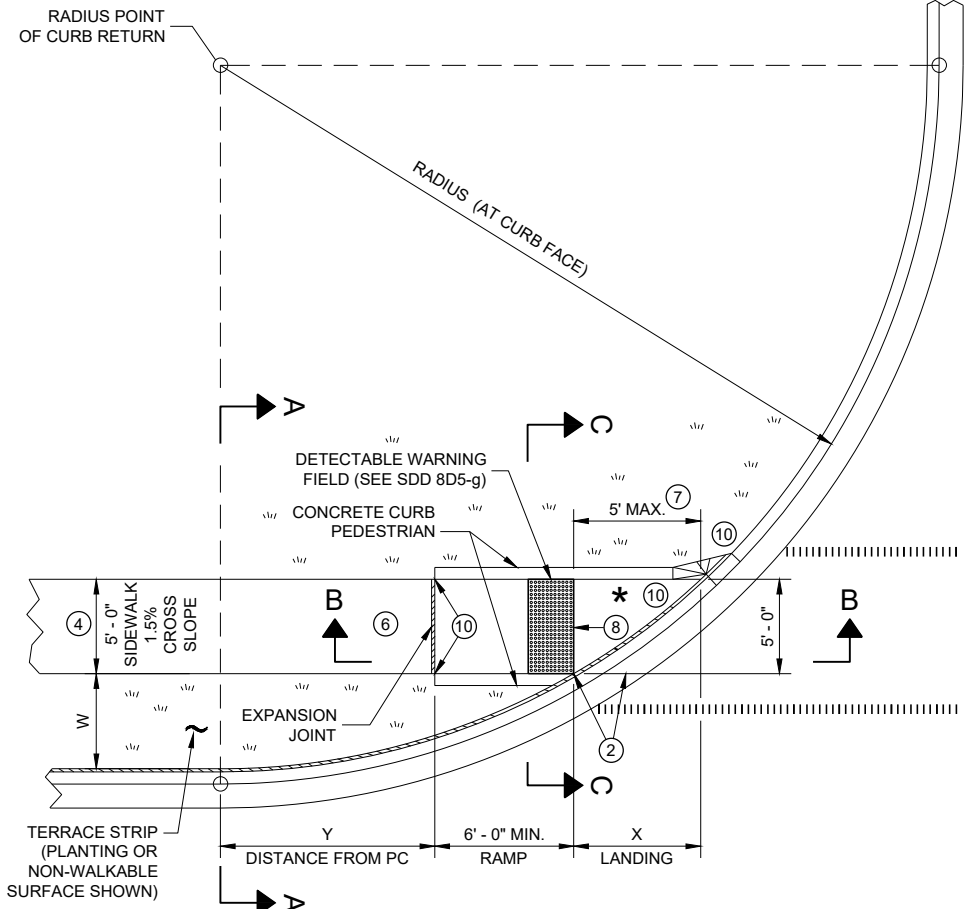
ISOMETRIC VIEW FOR TYPE 4A



ISOMETRIC VIEW FOR TYPE 4A1

**CURB RAMPS
TYPE 4A AND 4A1**

STATE OF WISCONSIN
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PLAN VIEW CURB RAMP TYPE 4B

RADIUS (AT CURB FACE)	W = 3' - 0"		W = 4' - 0"		W = 5' - 0"		W = 6' - 0"		W = 7' - 0"		W = 8' - 0"		W = 9' - 0"		W = 10' - 0"	
	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y
10 FEET	2' - 10 1/4"	0' - 5"	2' - 1"	1' - 4 1/2"	1' - 5"	2' - 1"	0' - 10"	2' - 7 1/2"	0' - 3 1/4"	3' - 0 1/4"						
15 FEET	4' - 6 3/4"	2' - 1 3/4"	3' - 9"	3' - 5 3/4"	3' - 1 1/4"	4' - 6"	2' - 6 3/4"	5' - 4 1/2"	2' - 1"	6' - 1"	1' - 8"	6' - 8 1/2"	1' - 3 1/4"	7' - 2 1/2"	0' - 10 3/4"	7' - 7 1/4"
20 FEET	5' - 9 3/4"	3' - 6 1/2"	4' - 11 1/2"	5' - 1 3/4"	4' - 3 1/4"	6' - 5 1/2"	3' - 8 3/4"	7' - 7"	3' - 3"	8' - 6 1/2"	2' - 10"	9' - 4 1/2"	2' - 5 1/2"	10' - 1 1/4"	2' - 1 1/4"	10' - 9"
30 FEET			6' - 9 1/4"	7' - 11 1/4"	6' - 0 1/4"	9' - 8"	5' - 5"	11' - 1 3/4"	4' - 10 3/4"	12' - 5 3/4"	4' - 5 1/2"	13' - 7 3/4"	4' - 0 3/4"	14' - 8 1/2"	3' - 8 1/2"	15' - 8 1/4"
40 FEET									6' - 1 3/4"	15' - 8 1/2"	5' - 8"	17' - 2"	5' - 3"	18' - 5 3/4"	4' - 10 3/4"	19' - 8 1/4"
50 FEET															5' - 10 1/4"	23' - 2"

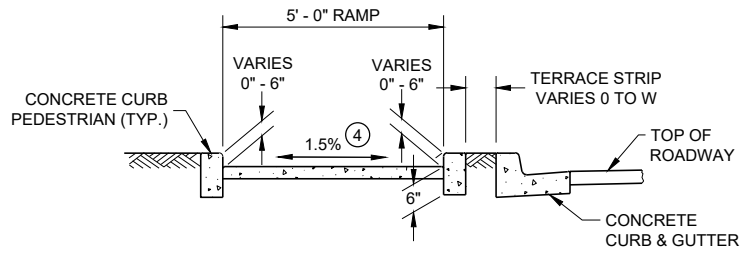
INTERMEDIATE RADII CAN BE INTERPOLATED
 DIMENSION "Y" IS CALCULATED BASED ON 6'-0" RAMP LENGTH
 DIMENSION "X" IS CALCULATED BASED ON 5'-0" SIDEWALK WIDTH

LEGEND

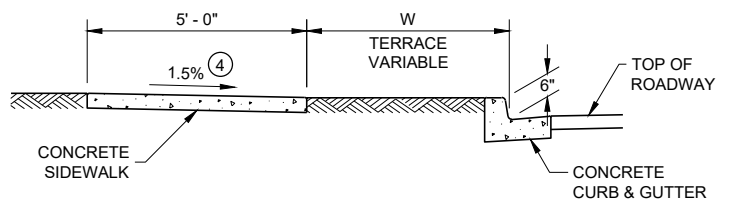
- 1/2" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT SIDEWALK
- PAVEMENT MARKING CROSSWALK (WHITE)

GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/2 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑦ WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑩ INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

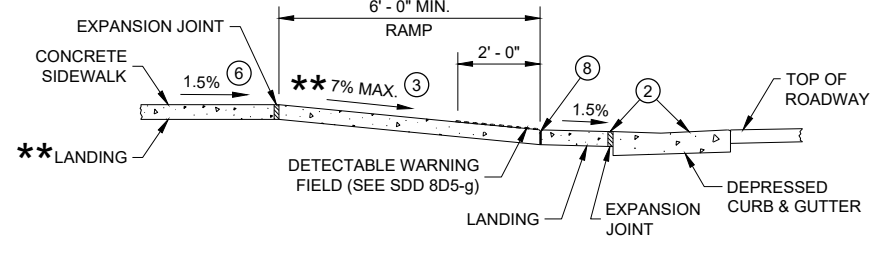


SECTION C - C FOR TYPE 4B



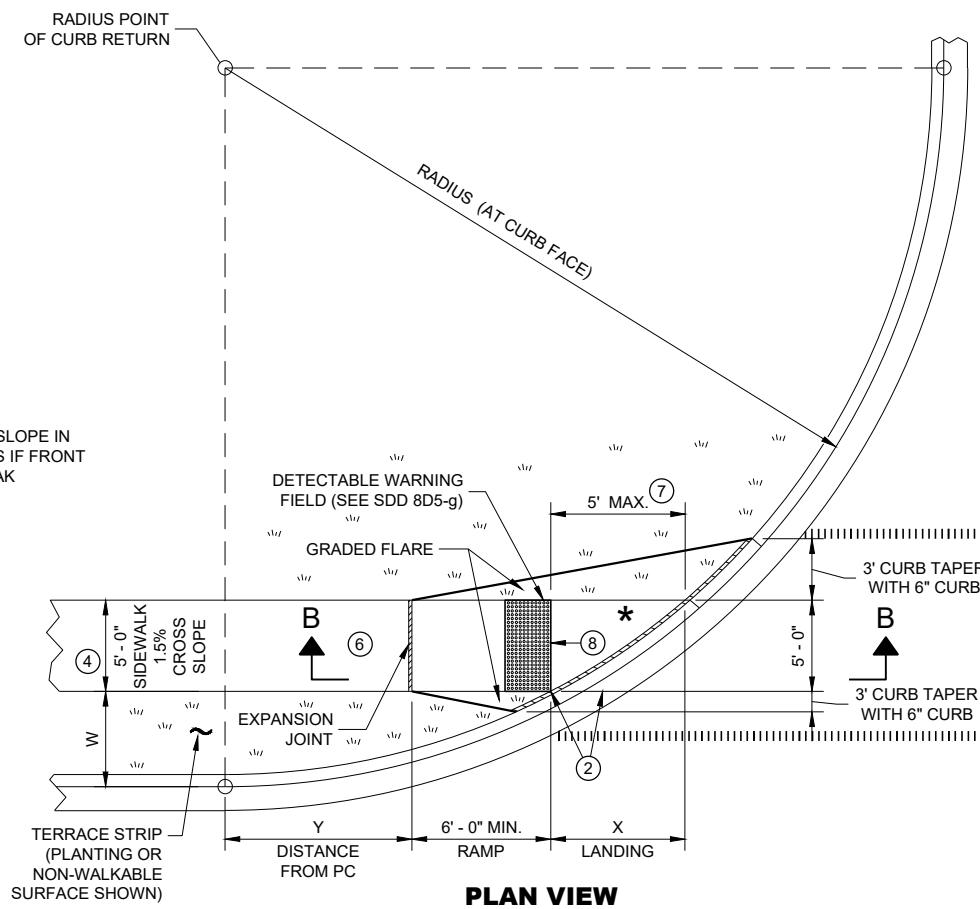
SECTION A - A FOR TYPE 4B

* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IF FRONT OF GRADE BREAK

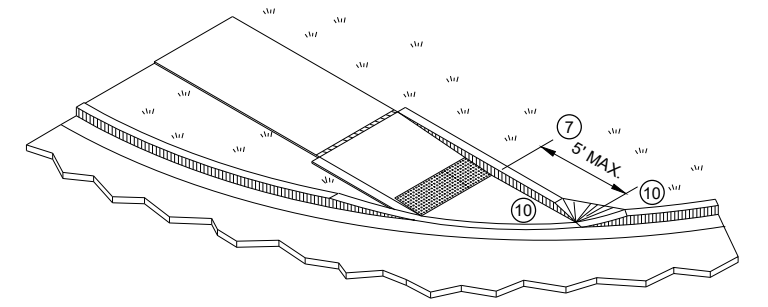


SECTION B - B FOR TYPE 4B AND TYPE 4B1

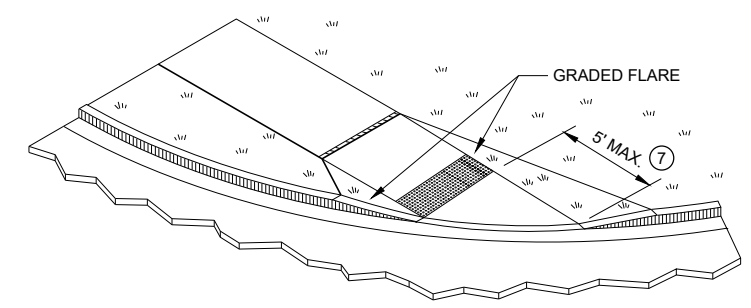
** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED



PLAN VIEW CURB RAMP TYPE 4B1



ISOMETRIC VIEW FOR TYPE 4B



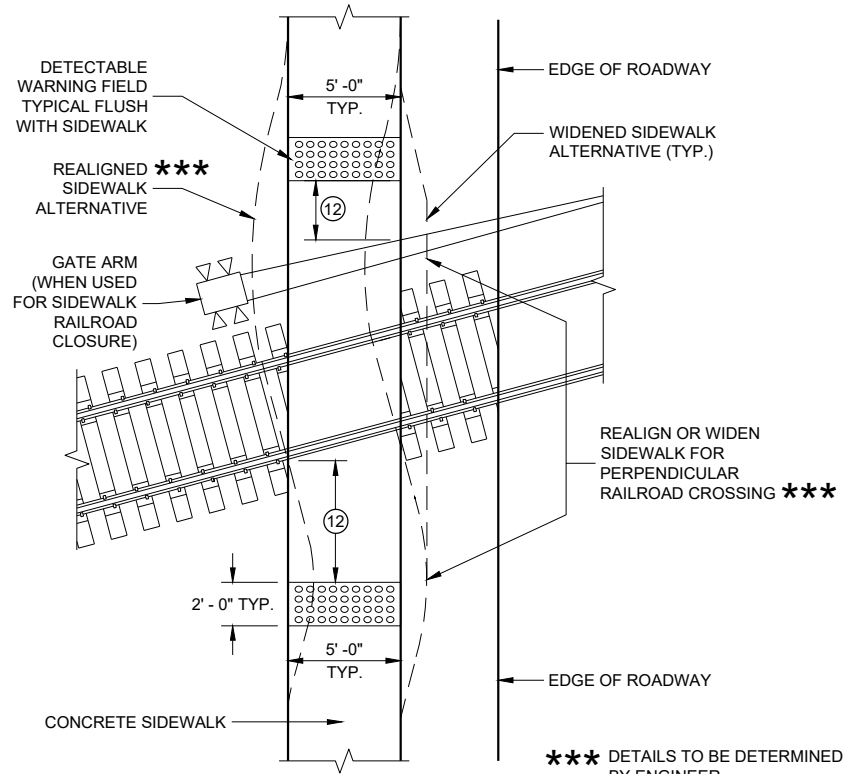
ISOMETRIC VIEW FOR TYPE 4B1

**CURB RAMPS
TYPE 4B AND 4B1**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

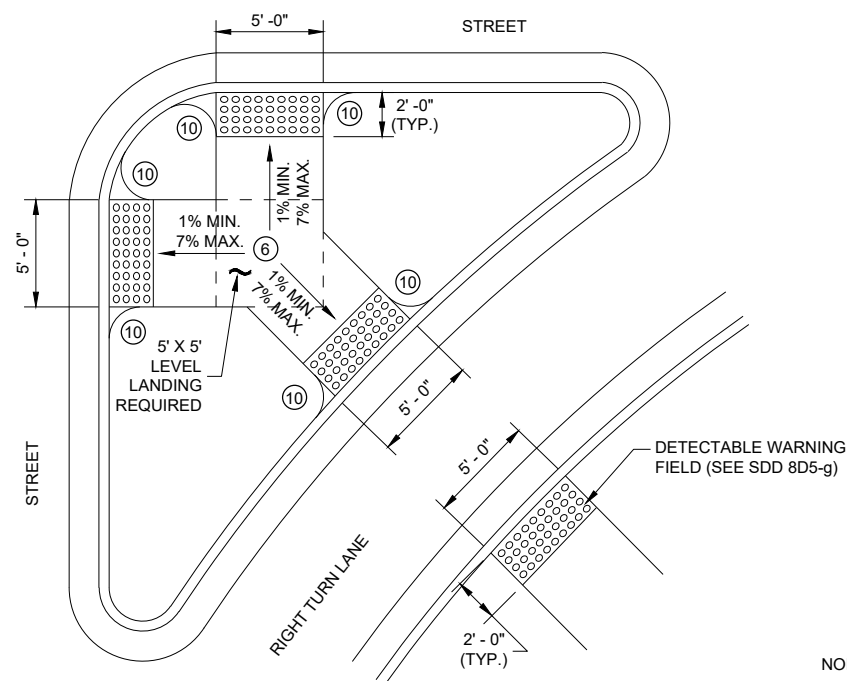
SDD 08D05 - 20d

SDD 08D05 - 20d



CURB RAMP TYPE 8

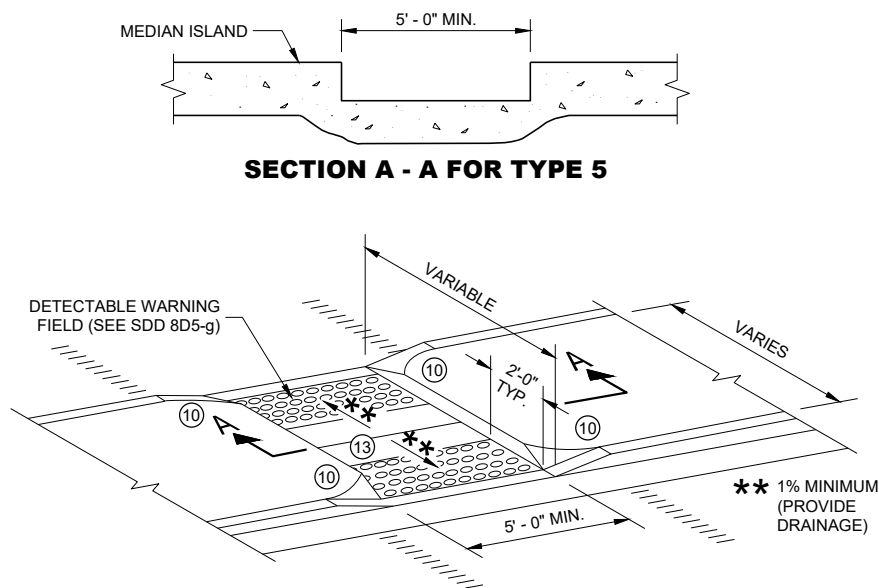
DETECTABLE WARNINGS AT RAILROAD CROSSING



CURB RAMP TYPE 6

DETECTABLE WARNING AT ISLANDS

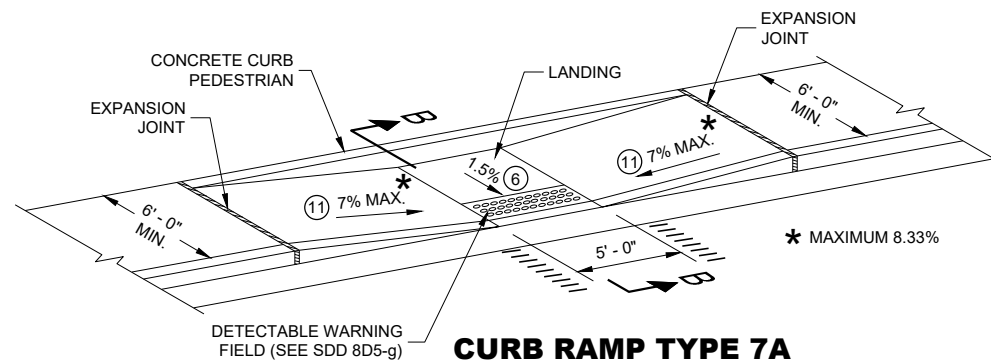
REFER TO GENERAL NOTES (2) AND (3) FOR ALL ISLAND CURB RAMPS



SECTION A - A FOR TYPE 5

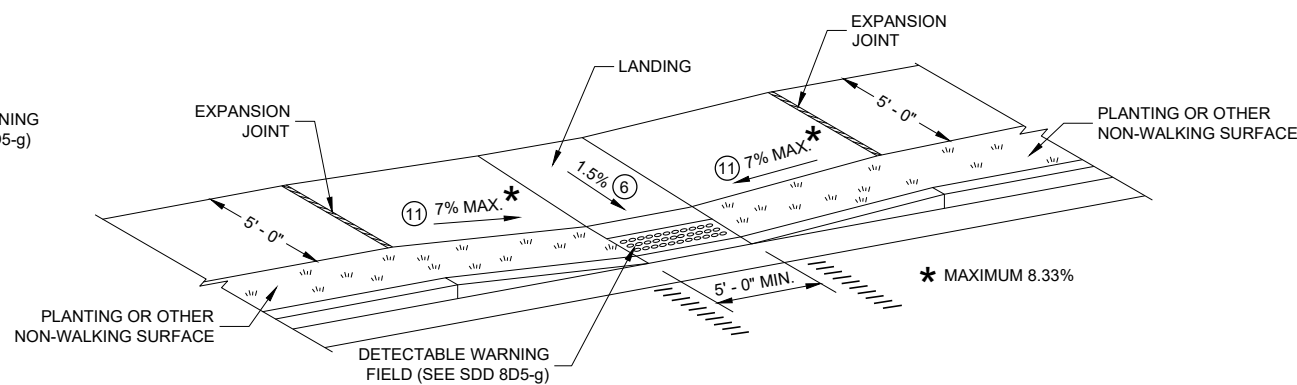
CURB RAMP TYPE 5

**MEDIAN ISLAND
NON-ELEVATED PEDESTRIAN CROSSING**



CURB RAMP TYPE 7A

MID BLOCK CROSSING



CURB RAMP TYPE 7B

MID BLOCK CROSSING

NOTE: THESE PARALLEL AND PARALLEL/PERPENDICULAR CURB RAMPS MAY BE USED AT INTERSECTIONS AND MID BLOCK LOCATIONS.

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

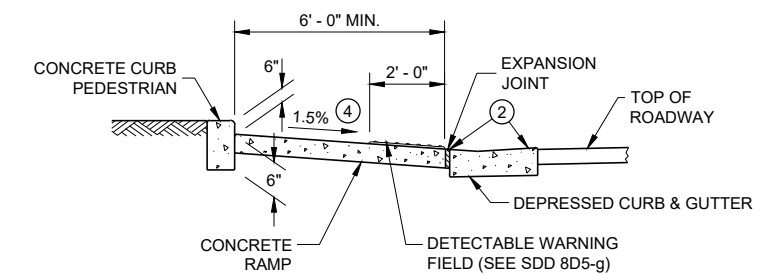
SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

- (2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- (3) AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- (4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- (6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- (10) INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.
- (11) SLOPE SIDEWALK TOWARD LANDING AS SHOWN WHERE THERE IS NO TERRACE OR WHERE THE TERRACE WIDTH IS LESS THAN 6 FEET WIDE.
- (12) THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO A RAILROAD CROSSING SHALL BE 1.5 FEET ±0.1' FROM THE FACE OF THE GATE ARM IF THE GATE ARM EXTENDS ACROSS THE SIDEWALK. WHERE THERE IS NO PEDESTRIAN GATE, THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO THE RAILROAD CROSSING SHALL BE 15 FEET FROM THE NEAREST RAIL.
- (13) DO NOT INSTALL DETECTABLE WARNING FIELDS AT THE EDGES OF STREET-LEVEL PEDESTRIAN REFUGE ISLANDS IF A MINIMUM 2 FOOT CONCRETE SURFACE WITHOUT DETECTABLE WARNINGS (MEASURED IN THE DIRECTION OF PEDESTRIAN TRAVEL) CANNOT BE ACHIEVED.

LEGEND

- ===== 1/2" EXPANSION JOINT SIDEWALK
- - - - - CONTRACTION JOINT FIELD LOCATED
- ||||||| PAVEMENT MARKING CROSSWALK (WHITE)

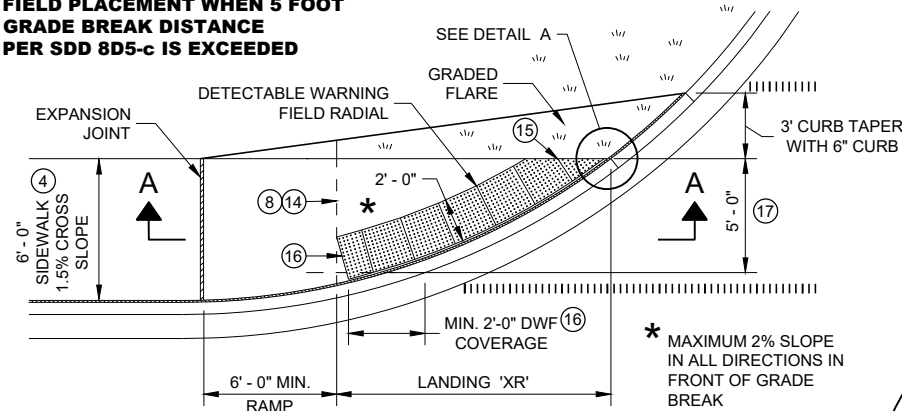


SECTION B - B FOR TYPE 7A

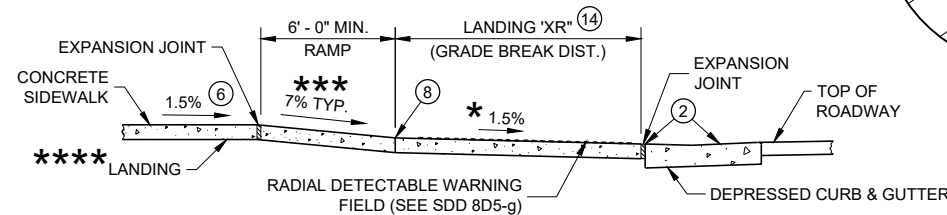
**CURB RAMPS
TYPE 5, 6, 7A, 7B & 8**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-c IS EXCEEDED



PLAN VIEW CURB RAMP TYPE 4A1 (GRADE BREAK DISTANCE GREATER THAN 5 FEET)

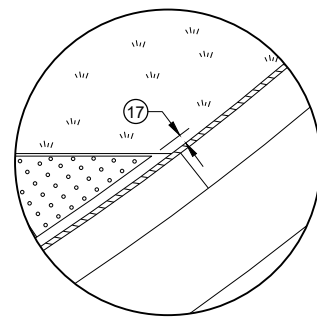


SECTION A - A FOR TYPE 4A1

**** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

*** MAXIMUM 8.33%

- LEGEND**
- 1/2" EXPANSION JOINT SIDEWALK
 - - - - - CONTRACTION JOINT SIDEWALK
 - ||||| PAVEMENT MARKING CROSSWALK (WHITE)

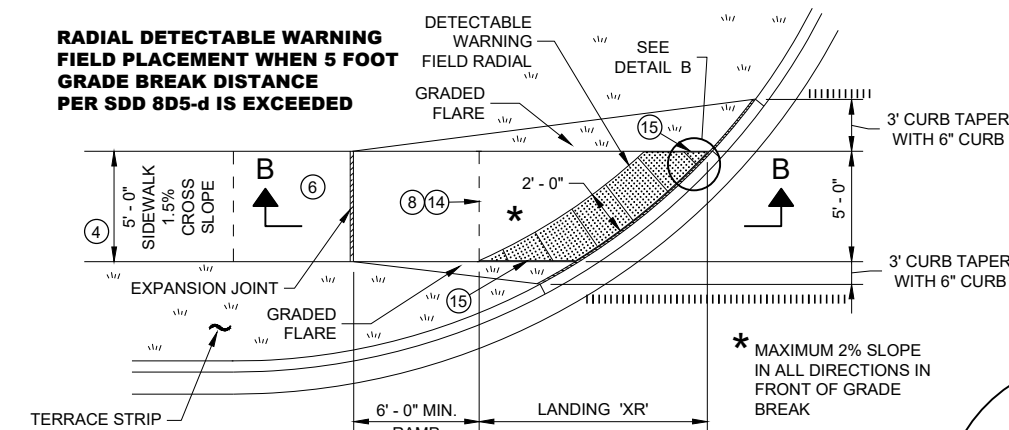


DETAIL A

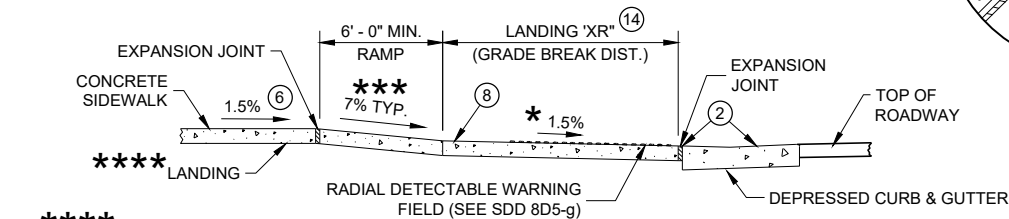
GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- APPLY RADIAL DETECTABLE WARNING PLACEMENT SIMILARLY FOR TYPE 4A AND 4A1 CURB RAMPS AND SIMILARLY FOR TYPE 4B AND 4B1 CURB RAMPS. TYPE 4A AND 4B RAMPS ARE NOT SHOWN.
- REFER TO SDD 8D5-g FOR ADDITIONAL RADIAL PLATE REQUIREMENTS.
- FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FILED ARE PROHIBITED.
- DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.
- 2 GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- 3 AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- 4 ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- 6 PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET BY 5 FEET.
- 8 PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- 14 CONSULT ENGINEER IF GRADE BREAK LOCATION (END OF LANDING DIMENSION "XR") REQUIRES FIELD ADJUSTMENT WHEN ESTABLISHING FINAL RADIAL DETECTABLE WARNING FIELD LOCATION.
- 15 FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN 1/8" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.
- 16 USE 1' X 2" RECTANGULAR END PLATE AT END OF TYPE 4A1 RAMP AND PROVIDE MINIMUM 2' - 0" DETECTABLE WARNING FIELD COVERAGE (IN DIRECTION OF PEDESTRIAN TRAVEL) ALONG THE ENTIRE CURB RAMP WIDTH.
- 17 A MAXIMUM 3 INCH CONCRETE BORDER WITH IS ALLOWABLE IN FROM OF RADIAL DETECTABLE WARNING FIELD FOR CONSTRUCTABILITY PURPOSES. CONCRETE BORDER WIDTH MAY VARY UP TO 1 INCH.

RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-d IS EXCEEDED



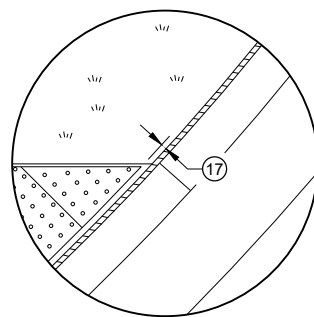
PLAN VIEW CURB RAMP TYPE 4B1 (GRADE BREAK DISTANCE GREATER THAN 5 FEET)



SECTION B - B FOR TYPE 4B1

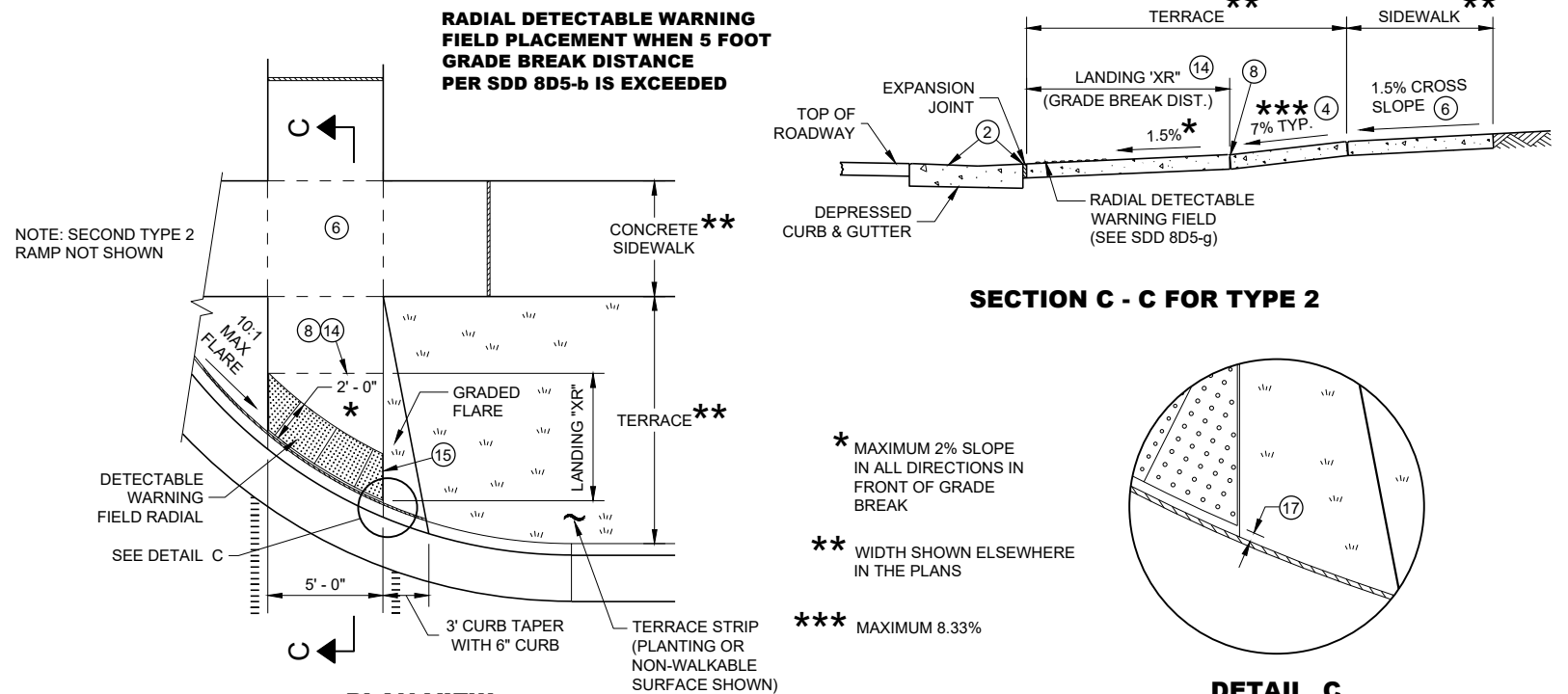
**** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

*** MAXIMUM 8.33%



DETAIL B

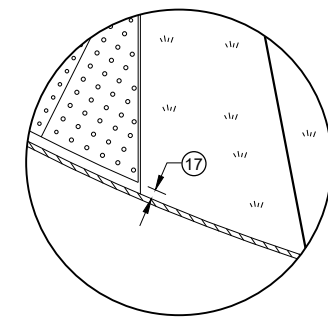
RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-b IS EXCEEDED



PLAN VIEW CURB RAMP TYPE 2 (GRADE BREAK DISTANCE GREATER THAN 5 FEET) (ON LINE WITH SIDEWALK)

NOTE: SECOND TYPE 2 RAMP NOT SHOWN

- * MAXIMUM 2% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK
- ** WIDTH SHOWN ELSEWHERE IN THE PLANS
- *** MAXIMUM 8.33%



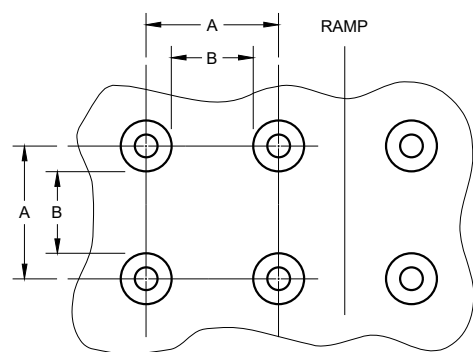
DETAIL C

CURB RAMPS RADIAL DETECTABLE WARNING FIELD APPLICATIONS

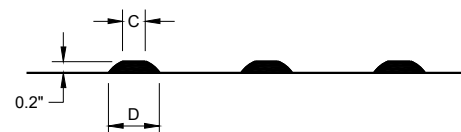
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

	MIN.	MAX.
A	1.6"	2.4"
B	0.65"	1.5"
C	*	*
D	0.9"	1.4"

* THE C DIMENSION IS 50% TO 65% OF THE D DIMENSION.

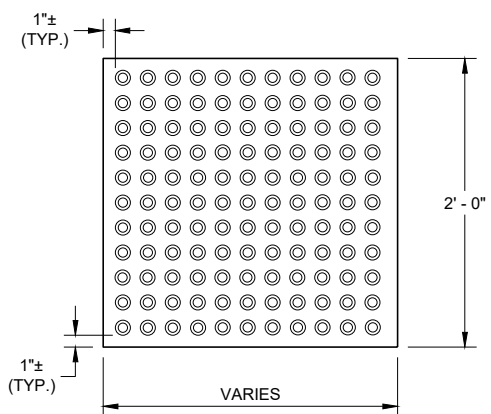


PLAN VIEW

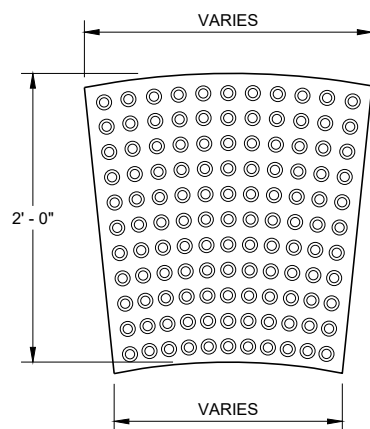


ELEVATION VIEW

**TRUNCATED DOMES
DETECTABLE WARNING PATTERN DETAIL**

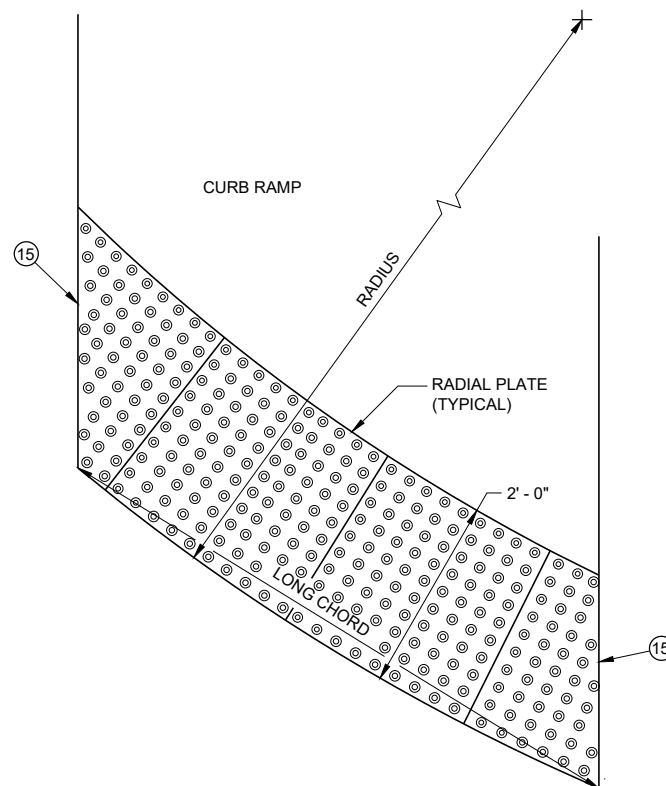


RECTANGULAR
PLATES

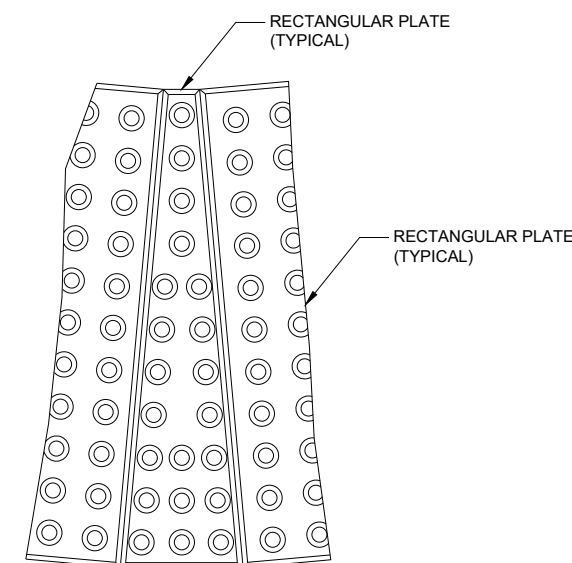


RADIAL
PLATES

PLAN VIEW
DETECTABLE WARNING FIELDS (TYPICAL)



PLAN VIEW
RADIAL DETECTABLE
WARNING FIELD ATTRIBUTES



PLAN VIEW
RADIAL WEDGE PLATE
CONNECTION DETAIL

GENERAL NOTES

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AT A CURB RAMP SHALL BE FROM THE SAME MANUFACTURER.

PLACE ALL DETECTABLE WARNING FIELD SYSTEMS IN ACCORDANCE TO THE MANUFACTURER'S RECOMMENDATION.

FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FIELD ARE PROHIBITED.

DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.

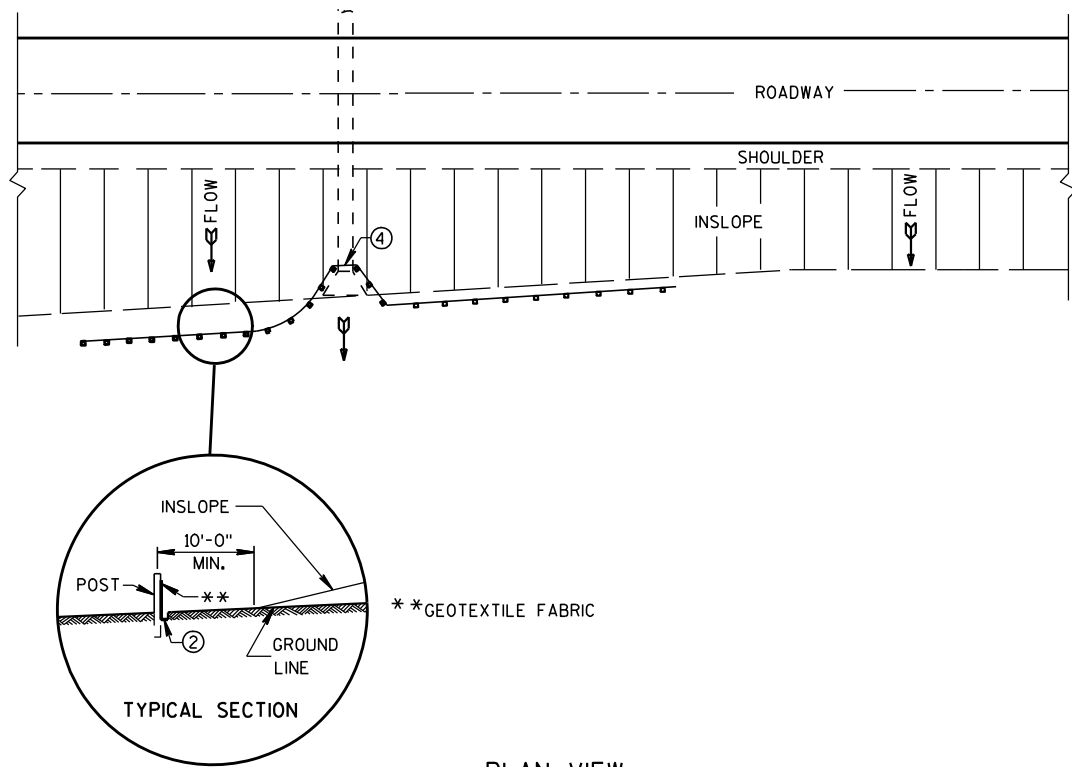
FOR RADIAL DETECTABLE WARNING FIELD APPLICATIONS WHERE STANDARD RADIAL PLATES ARE NOT AVAILABLE AT AN INTERSECTION CURB RADIUS, A COMBINATION OF SQUARE OR RECTANGULAR PLATES AND RADIAL PLATES MAY BE USED TO FORM RADIAL CONFIGURATION. RADIAL WEDGE PLATES IN COMBINATION WITH SQUARE PLATES ARE ALSO ACCEPTABLE. FOLLOW MANUFACTURER'S RECOMMENDATIONS.

REFER TO CONTRACT AND STANDARD SPECIFICATIONS FOR FIELD CUTTING REQUIREMENTS.

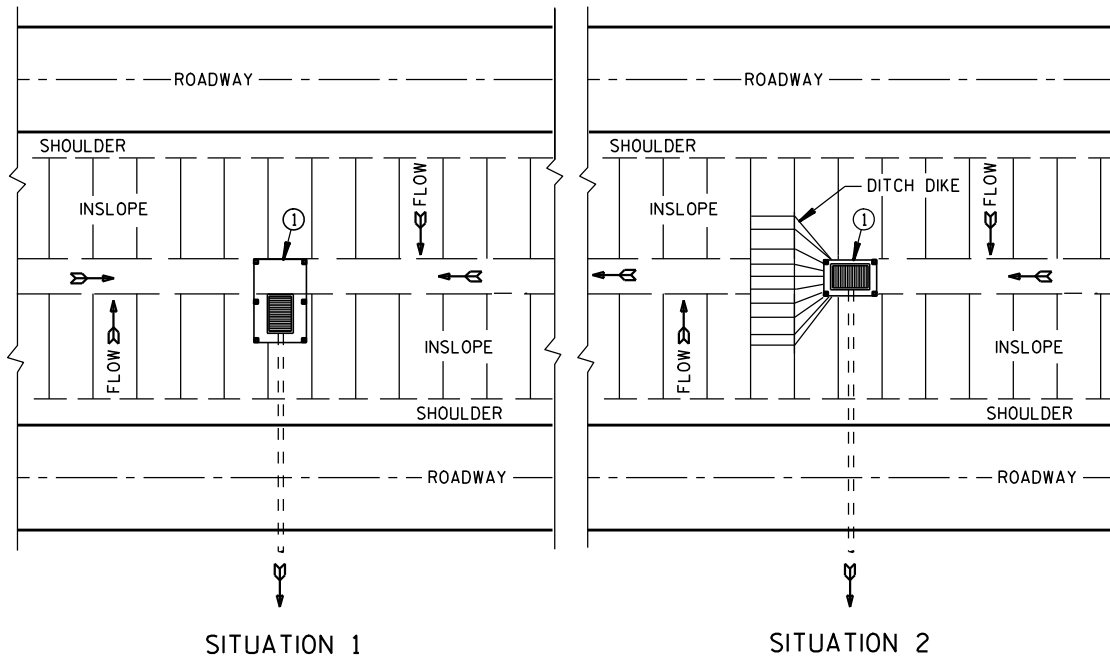
DO NOT EMBED IN CONCRETE ANY FIELD-CUT PLATES WITH CUT EDGES SHORTER THAN 6 INCHES. CONSULT WITH MANUFACTURER FOR RE-DRILLING AND ANCHORING REQUIREMENTS OF FIELD-CUT PLATES.

15 FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN 1/8" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.

CURB RAMPS RECTANGULAR AND RADIAL DETECTABLE WARNING PLATES	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2019 DATE	/S/ Rodney Taylor ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR
<small>FHWA</small>	



PLAN VIEW
TYPICAL APPLICATION OF SILT FENCE

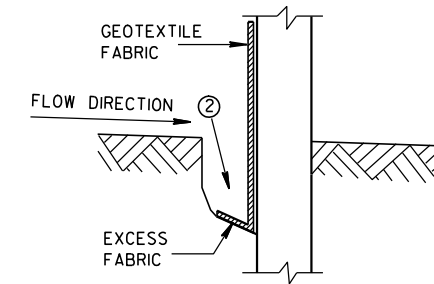


SITUATION 1 SITUATION 2
PLAN VIEW
SILT FENCE AT MEDIAN SURFACE DRAINS

GENERAL NOTES

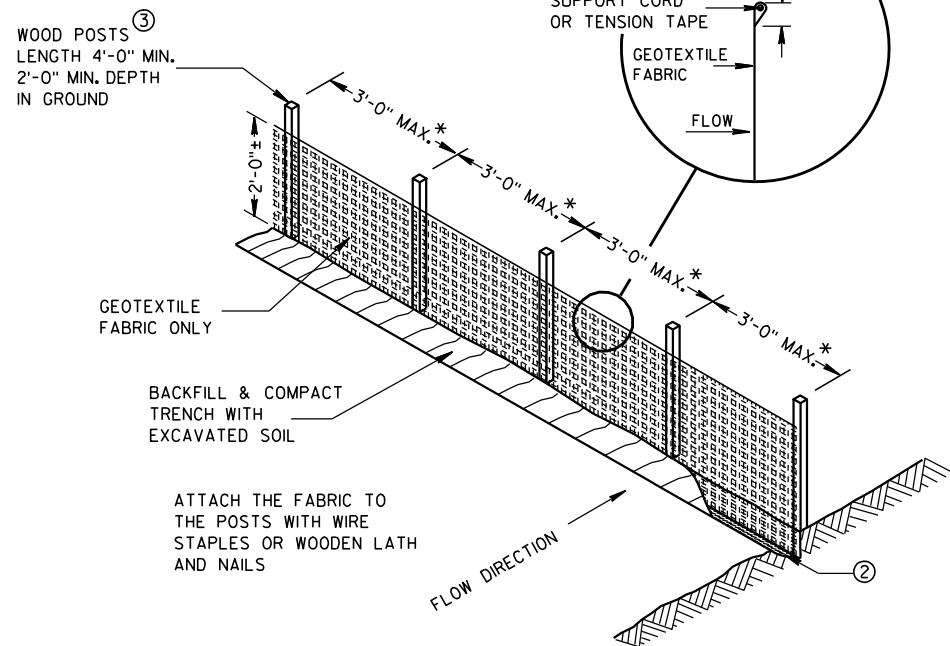
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

- ① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- ③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1 1/8" X 1 1/8" OF OAK OR HICKORY.
- ④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- ⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.

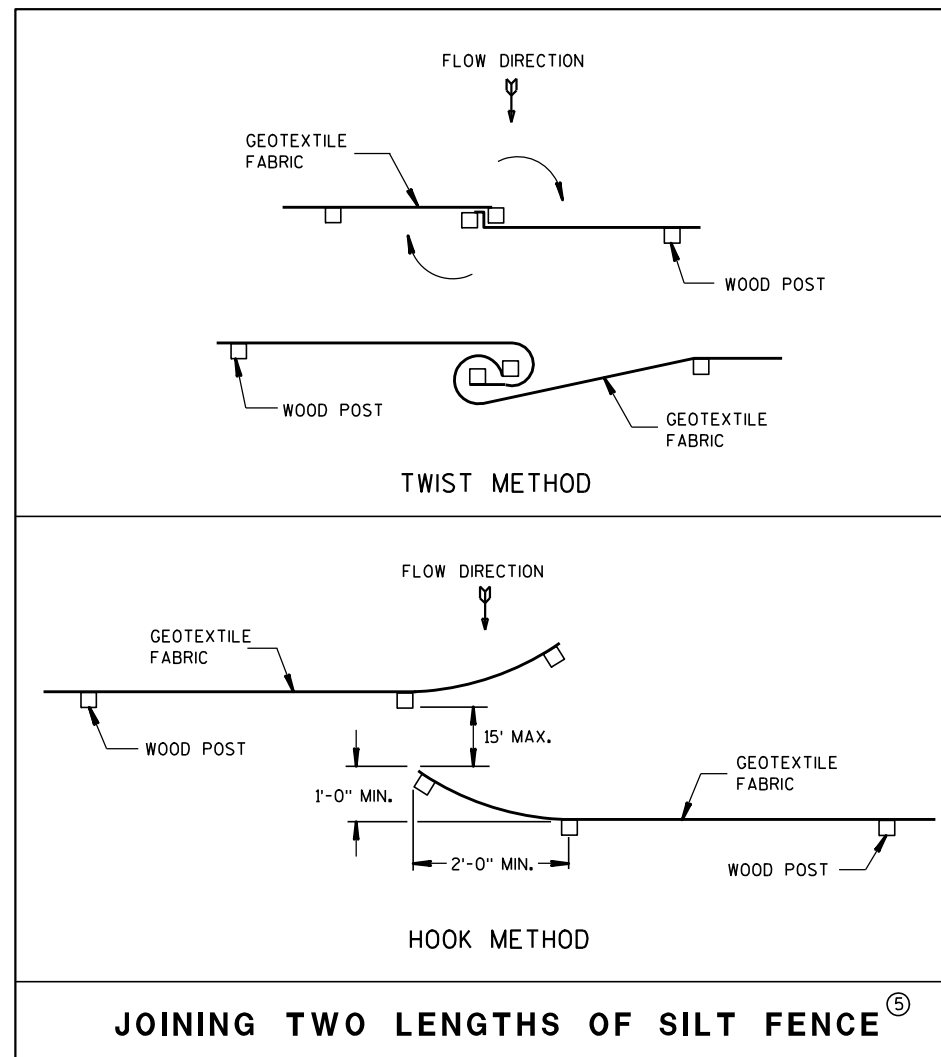


TRENCH DETAIL

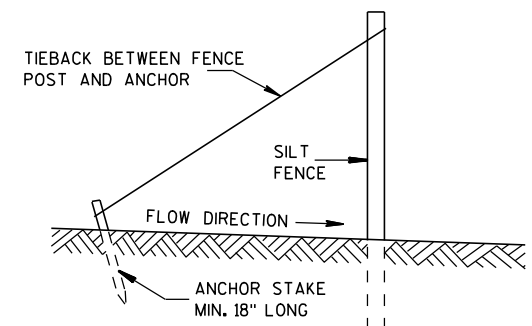
NOTE: ADDITIONAL POST DEPTH OR TIE BACKS MAY BE REQUIRED IN UNSTABLE SOILS



SILT FENCE



JOINING TWO LENGTHS OF SILT FENCE ⑤

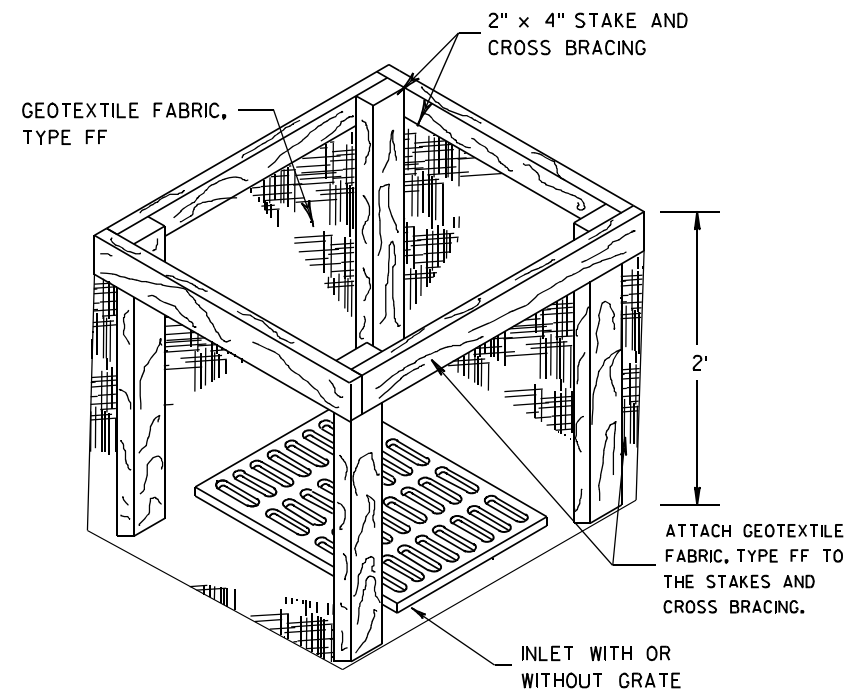
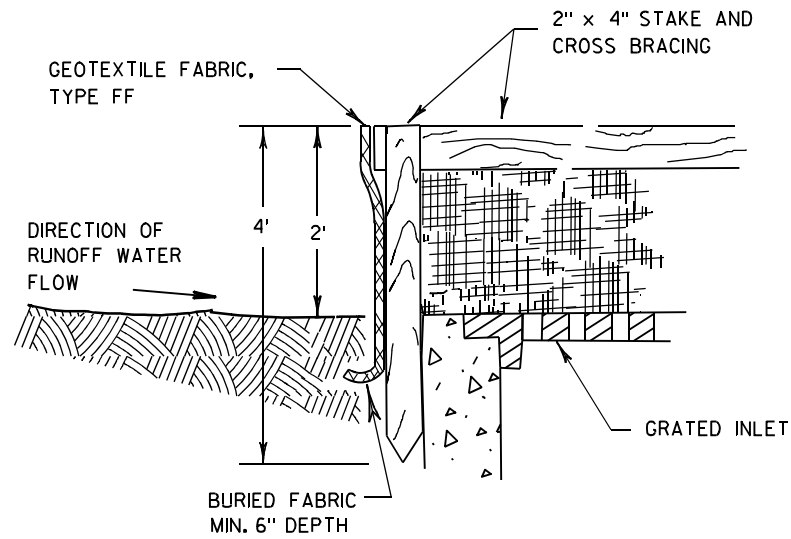


SILT FENCE TIE BACK
(WHEN REQUIRED BY THE ENGINEER)

SILT FENCE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
4-29-05 /S/ Beth Canestra
DATE CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA



INLET PROTECTION, TYPE A

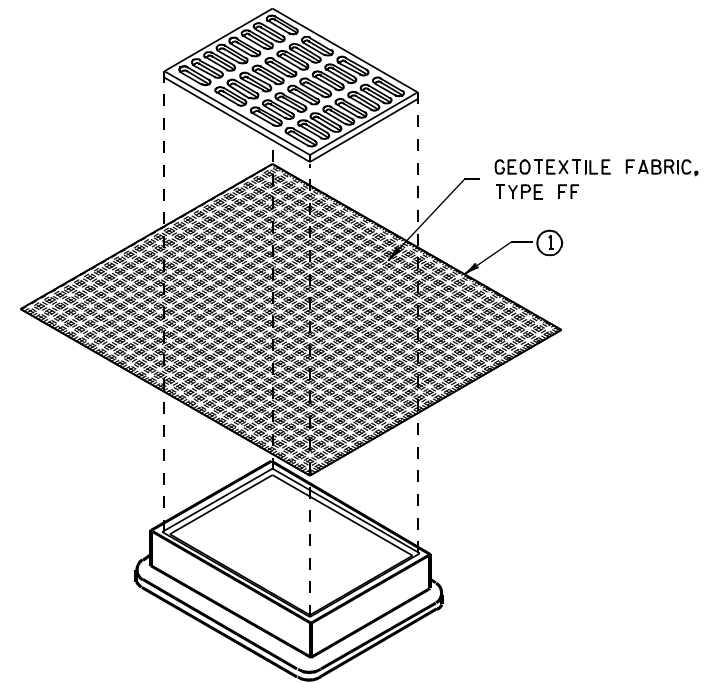
GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

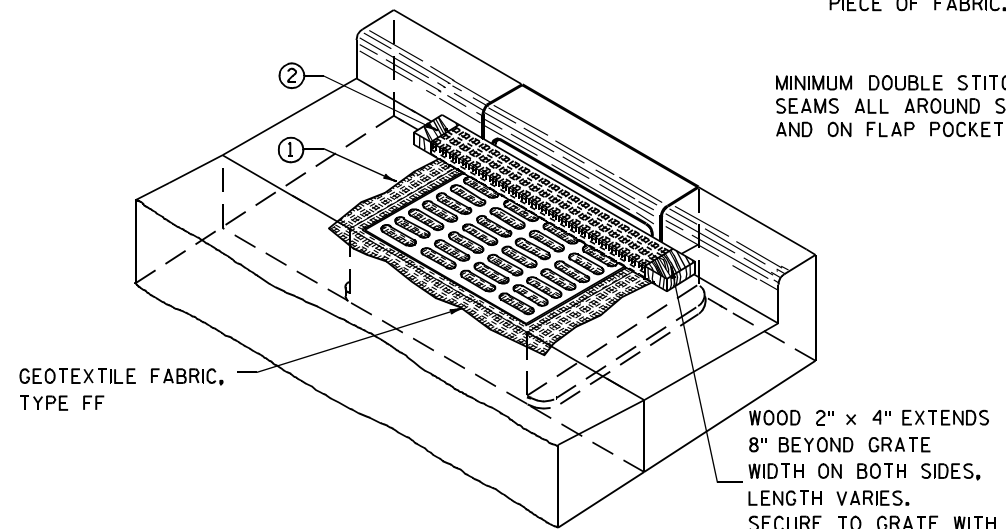
WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- ① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- ② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- ③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



**INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)**

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE C (WITH CURB BOX)

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

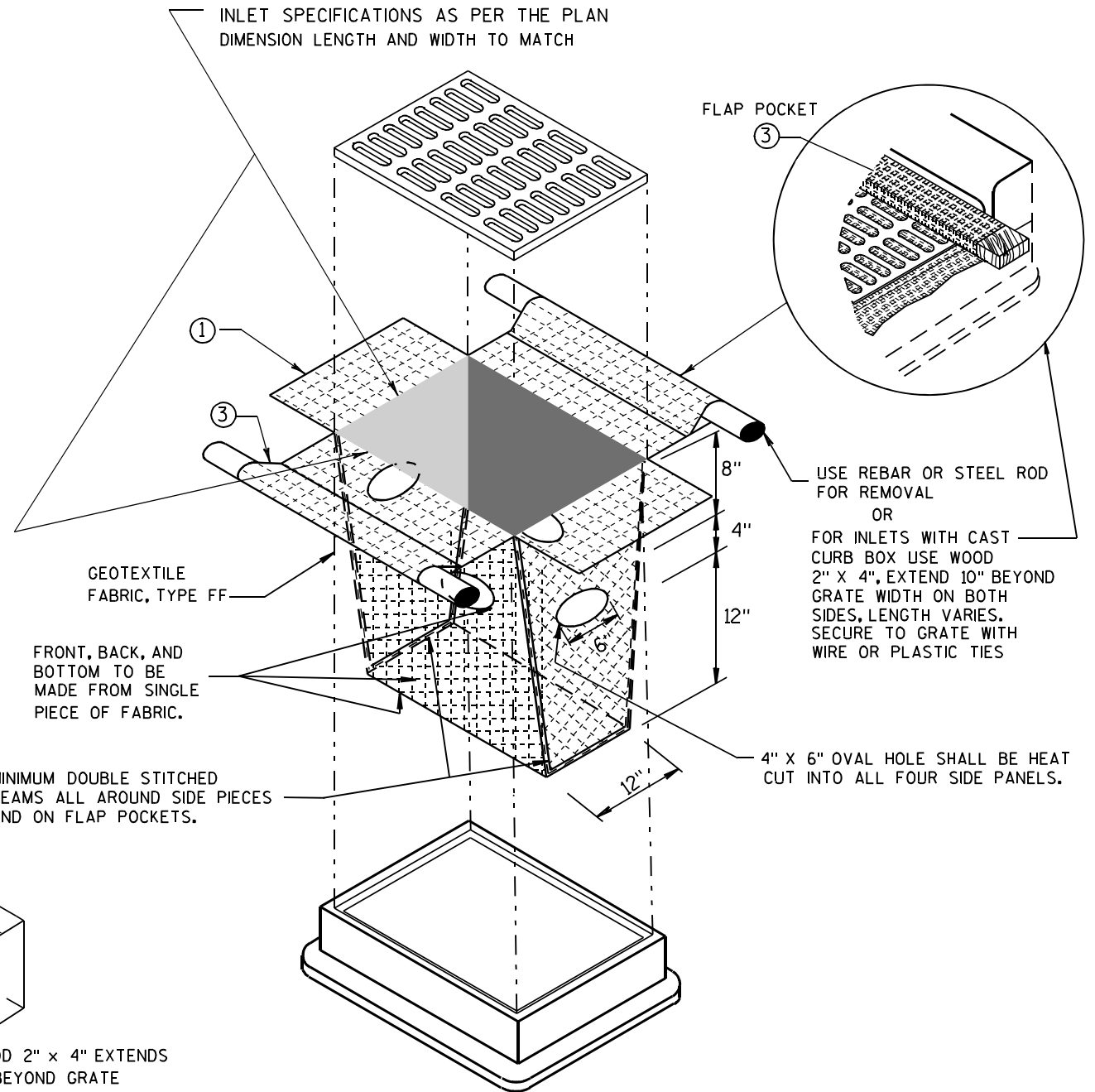
THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

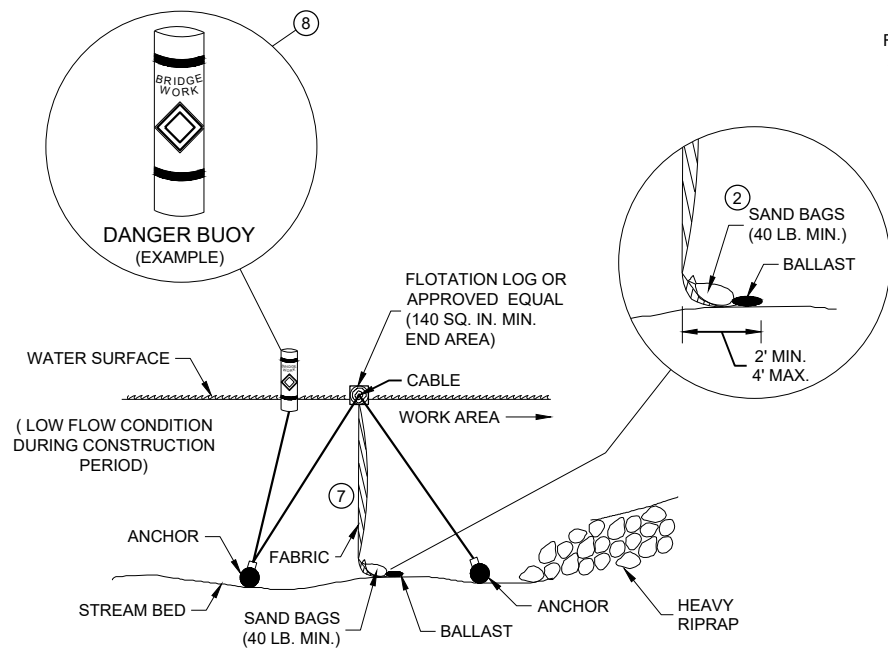
THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.



INLET PROTECTION, TYPE D

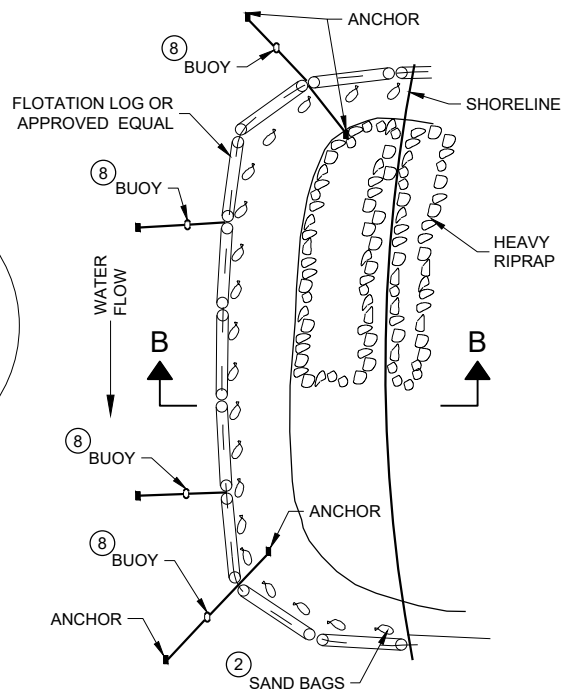
(CAN BE INSTALLED IN ANY INLET TYPE WITH OR WITHOUT A CURB BOX AS PER NOTE ②)

INLET PROTECTION TYPE A, B, C, AND D	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 10/16/02 DATE	/s/ Beth Connestra CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA	

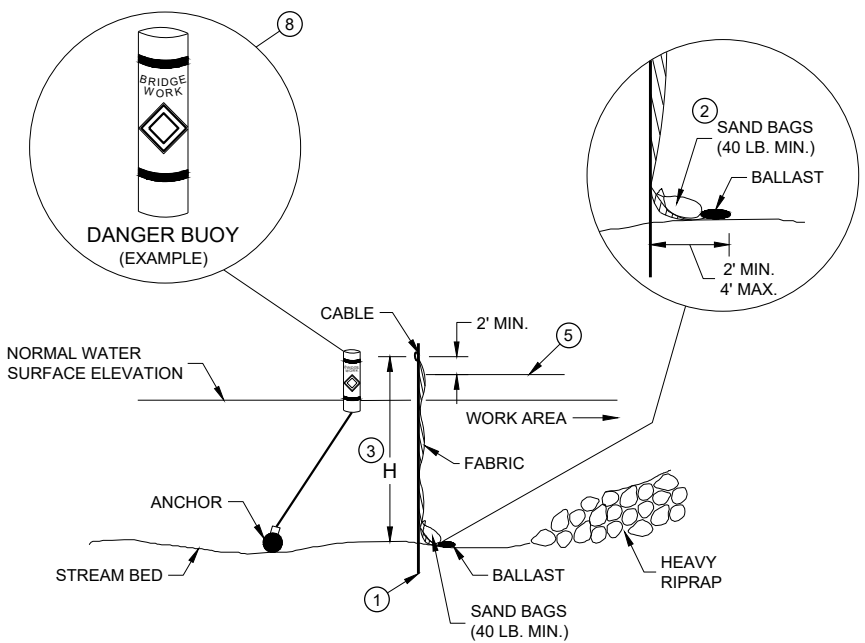


SECTION B - B

**TURBIDITY BARRIER - FLOAT ALTERNATIVE
CAUTION - SEE NOTE 6**

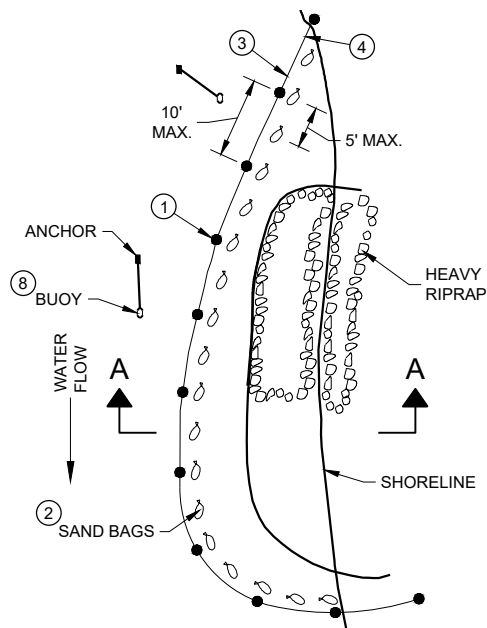


PLAN VIEW



SECTION A - A

TURBIDITY BARRIER - STANDARD POST INSTALLATION



PLAN VIEW

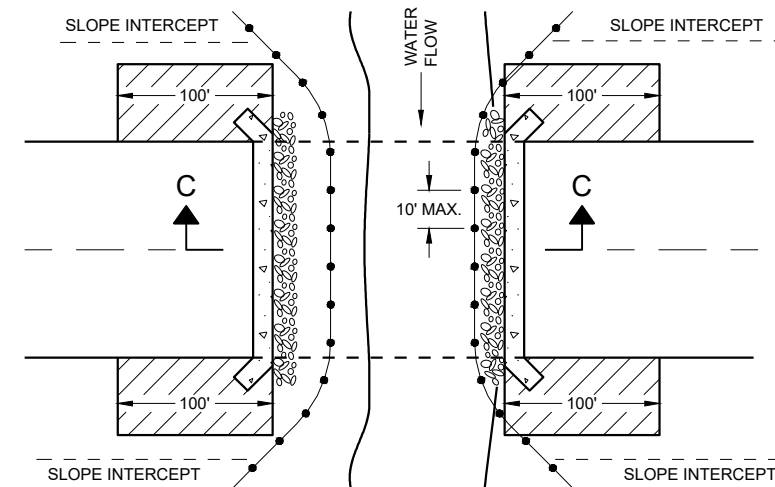
TURBIDITY BARRIER PLACEMENT DETAILS

GENERAL NOTES

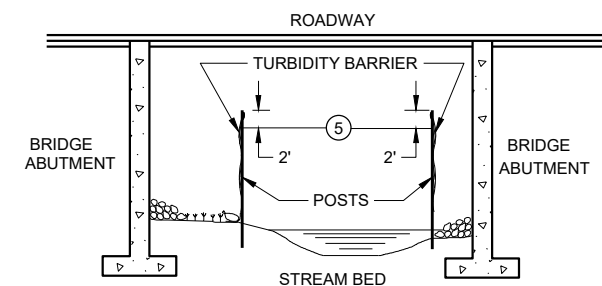
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

TURBIDITY BARRIER MAY BE REMOVED AT THE ENGINEERS DISCRETION, WHEN PERMANENT EROSION CONTROL MEASURES HAVE BEEN ESTABLISHED.

- ① DRIVEN STEEL POSTS, PIPES, OR CHANNELS. LENGTH SHALL BE SUFFICIENT TO SECURELY SUPPORT BARRIER AT HIGH WATER ELEVATIONS.
- ② SAND BAGS TO BE USED AS ADDITIONAL BALLAST WHEN ORDERED BY THE ENGINEER TO MEET ADVERSE FIELD CONDITIONS. SPACE AS APPROPRIATE FOR SITE CONDITIONS.
- ③ WHEN BARRIER HEIGHT "H" EXCEEDS 8 FEET, POST SPACING MAY NEED TO BE DECREASED.
- ④ IN WATERWAYS SUBJECT TO FLUCTUATING WATER ELEVATIONS, PROVISIONS SHOULD BE MADE TO ALLOW THE WATER TO EQUALIZE ON EACH SIDE OF THE BARRIER. THIS MAY BE ACCOMPLISHED BY LEAVING A PORTION OF THE BARRIER OPEN ON THE UPSTREAM END.
- ⑤ ESTIMATED HIGH WATER ELEVATION DURING CONSTRUCTION PERIOD. MINIMUM BARRIER HEIGHT SHALL BE 2' GREATER THAN EITHER THE Q2 ELEVATION OR THE ESTIMATED HIGH WATER ELEVATION DURING CONSTRUCTION, WHICHEVER IS GREATER.
- ⑥ FLOAT ALTERNATIVE WILL ONLY BE ALLOWED WITH WRITTEN APPROVAL OF THE ENGINEER, AND IS MEANT FOR LOCATIONS WHERE BEDROCK PREVENTS THE INSTALLATION OF POSTS.
- ⑦ ALLOW SUFFICIENT SLACK VERTICALLY AND HORIZONTALLY SO THAT SEDIMENT BUILD UP WILL NOT SEPARATE OR LOWER THE TURBIDITY BARRIER.
- ⑧ USE AS DIRECTED BY COAST GUARD OR DNR PERMIT WHEN WORKING IN NAVIGABLE WATERWAYS.



PLAN VIEW



SECTION C - C

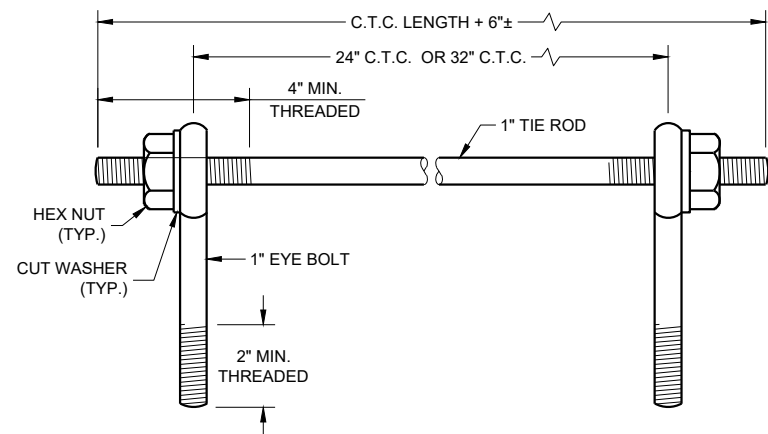
**TURBIDITY BARRIER DETAIL SHOWING
TYPICAL PLACEMENT AT STRUCTURES**

TURBIDITY BARRIER

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

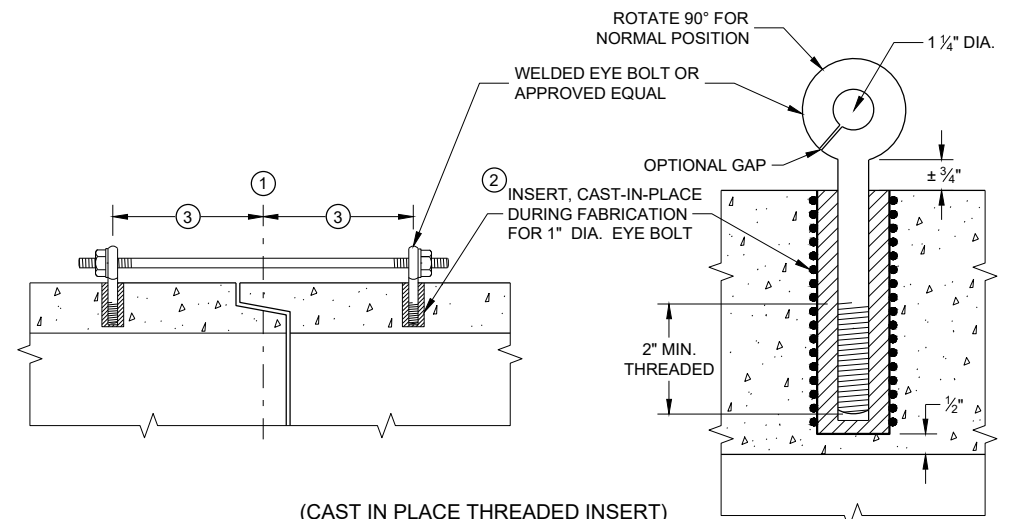
APPROVED
6/4/02 DATE /S/ Beth Cannestra
DATE CHIEF ROADWAY DEVELOPMENT
ENGINEER

FHWA



EYE BOLTS AND TIE ROD

EYE BOLT AND TIE ROD ASSEMBLY (ALTERNATE NO. 1)



(CAST IN PLACE THREADED INSERT)
LONGITUDINAL SECTIONS

GENERAL NOTES

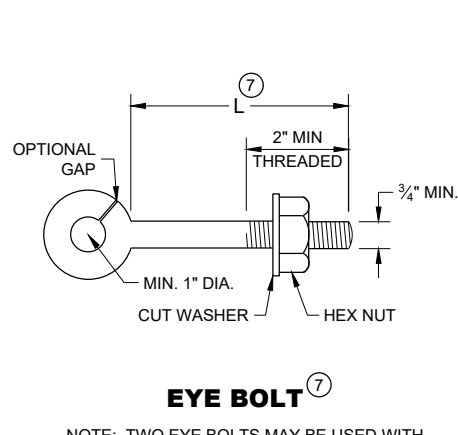
DETAILS OF CONSTRUCTION, MATERIALS, AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

CONCRETE CULVERT AND STORM SEWER PIPE SHALL BE TIED TOGETHER IN THE MANNER ILLUSTRATED BY THIS DETAIL AT LOCATIONS DESIGNATED IN THE STANDARD SPECIFICATIONS AND THE PLAN. THE CONTRACTOR MAY USE EITHER ALTERNATE 1, 2 OR 3 FOR DRAINAGE STRUCTURES. ONLY ALTERNATE 1 AND 3 MAY BE USED FOR CATTLE PASSES, UNLESS OTHERWISE STATED IN THE CONTRACT. THE MATERIALS, FABRICATION AND WORK NECESSARY TO TIE THE PIPE BY THIS DETAIL WILL BE CONSIDERED INCIDENTAL TO THE PIPE AND APRON ENDWALLS IF REQUIRED.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR JOINT TIES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.

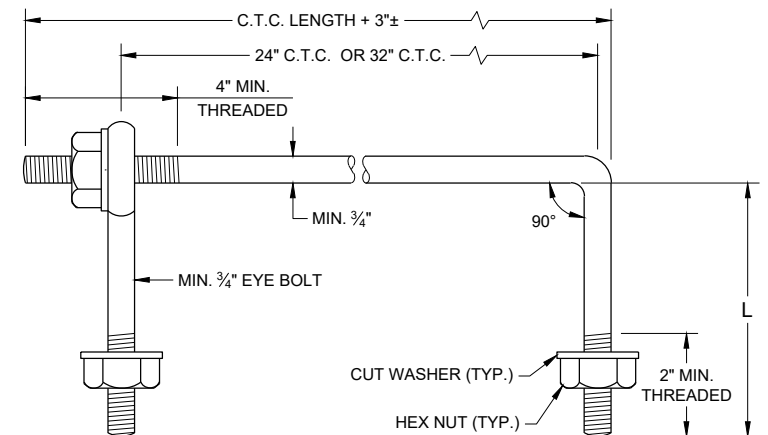
JOINT TIES TO BE HOT-DIP GALVANIZED PER ASTM A 153.

- ① CENTER LINE OF TONGUE AND GROOVE OR BELL AND SPIGOT JOINTS.
- ② THE INSIDE OF THE THREADED INSERTS SHALL BE CLEAN TO ALLOW THE INSERTION OF THREADED EYE BOLTS.
- ③ HOLES SHALL BE CAST-IN-PLACE OR DRILLED PER THE APPLICABLE DETAIL, AND EQUAL DISTANCE FROM THE CENTERLINE OF THE JOINT.
- ④ BOLT PROJECTION INSIDE OF PIPE SHALL NOT EXCEED 2 INCHES.
- ⑤ OPENING TO BE ROD DIAMETER PLUS 1 INCH.
- ⑥ LENGTH ADEQUATE TO EXTEND TO WITHIN 1/2 INCH OF THE INNER SURFACE OF THE PIPE.
- ⑦ EYE BOLT LENGTH DETERMINED BY WALL THICKNESS, BELL THICKNESS AND BOLT PROJECTION INSIDE PIPE.

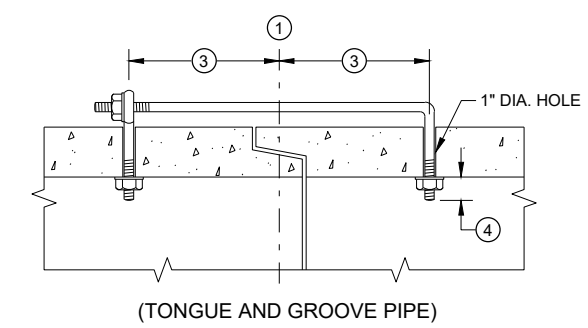


EYE BOLT ⑦

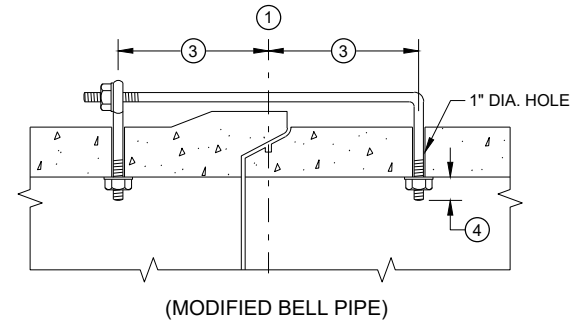
NOTE: TWO EYE BOLTS MAY BE USED WITH A 30\"/>



EYE BOLT AND TIE ROD



(TONGUE AND GROOVE PIPE)



(MODIFIED BELL PIPE)

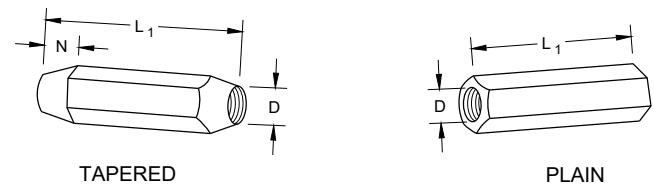
LONGITUDINAL SECTION
(JOINT TIES FOR 18\"/>

EYE BOLT AND TIE ROD ASSEMBLY (ALTERNATE NO. 2)

ADJUSTABLE TIE ROD TABLE

PIPE DIAMETER	TIE ROD DIAMETER	D	L ₁	N
12 - 60	5/8	5/8	5	1/2
66 - 84	3/4	3/4	5	1/2
90 - 144	1	1	7	1 7/16

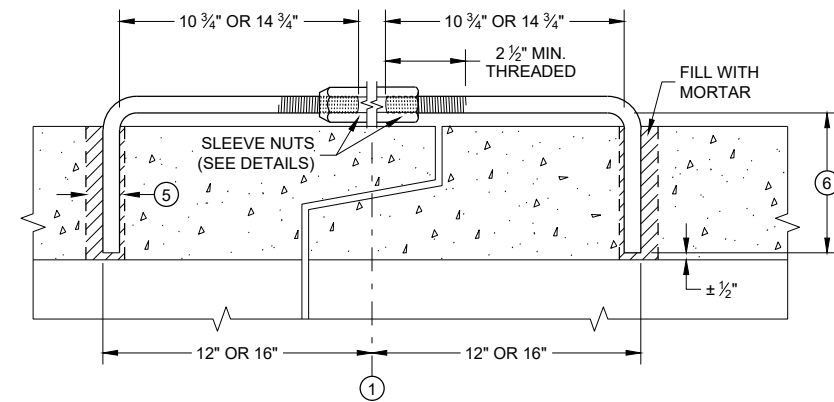
DIMENSIONS SHOWN ARE IN INCHES



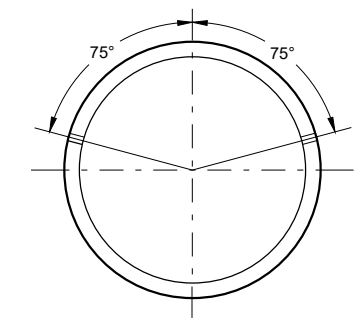
TAPERED

PLAIN

RIGHT AND LEFT THREADS SLEEVE NUTS

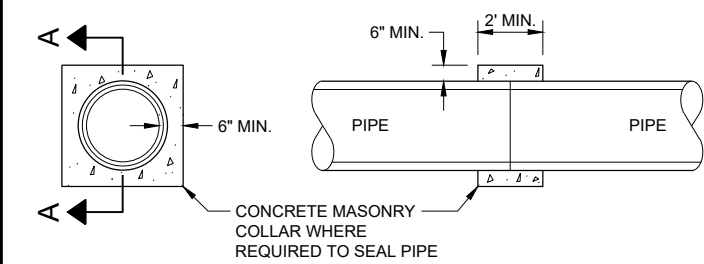


LONGITUDINAL SECTION
ADJUSTABLE TIE ROD (ALTERNATE NO. 3)



PLACEMENT OF (2) CAST-IN-PLACE INSERTS OR HOLES DURING FABRICATION FOR PIPE SECTIONS REQUIRING TIE RODS

TRANSVERSE SECTION



SECTION A - A
CONCRETE COLLAR DETAIL

JOINT TIES FOR CONCRETE PIPE AND CONCRETE COLLAR DETAIL

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2021 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT ENGINEER

FHWA

SDD 08F04 - 08

SDD 08F04 - 08

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

FILL SLOPES FLATTER THAN 2 1/2:1 SHALL BE WARPED TO MEET THE TOP OF THE WINGWALLS.

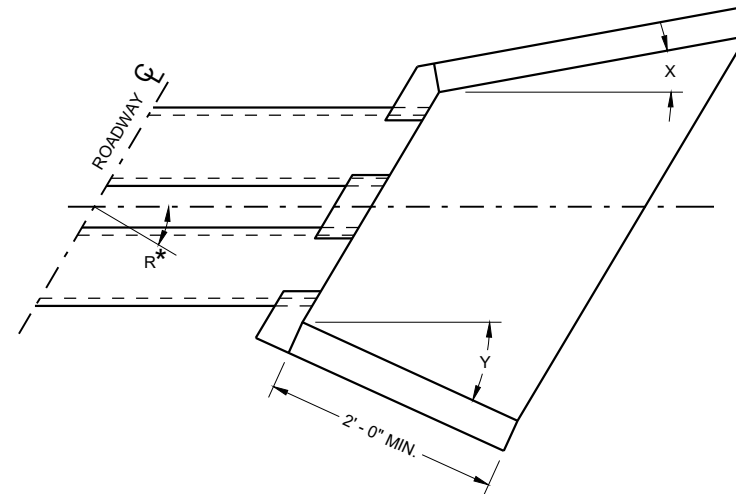
ALL STEEL REINFORCEMENT AND WELDED STEEL WIRE FABRIC SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE NOTED.

① MINIMUM REINFORCEMENT SHALL BE 6" X 6" - W4.0 X W4.0 OR NO. 3 BARS SPACED 12" C-C IN BOTH DIRECTIONS.

② THE SPACE BETWEEN PIPES SHALL BE AS FOLLOWS:

DIAMETER OR SPAN	SPACE
UP TO AND INCLUDING 48"	2' - 0"
OVER 48" TO 72"	1/2 DIA. OR SPAN
OVER 72"	3' - 0"

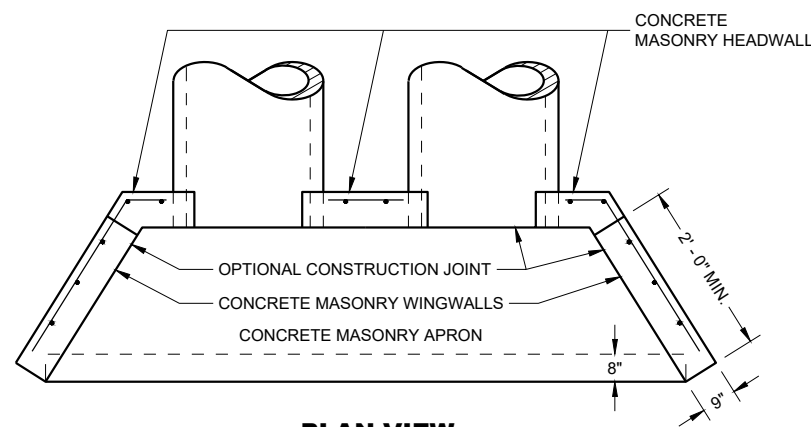
- ③ LIMITS OF TRENCH BACKFILL
- ④ LIMITS OF FOUNDATION BACKFILL
- ⑤ FOUNDATION AND TRENCH BACKFILL ARE MATERIALS INCLUDED IN PAYMENT FOR CULVERT PIPE, PIPE ARCH, CULVERT PIPE REINFORCED CONCRETE HORIZONTAL ELLIPSE OR CONCRETE MASONRY ENDWALLS.
- ⑥ DO NOT PLACE FOUNDATION BACK FILL OR ANY OTHER GRANULAR BACKFILL AROUND OR BELOW CUT OFF WALL. POUR CUT OFF WALL AGAINST NATIVE SOIL
- ⑦ MINIMUM HEIGHT SHALL BE THE GREATER OF 1'- 0" OR 1/4 HEIGHT OF CULVERT PIPE.



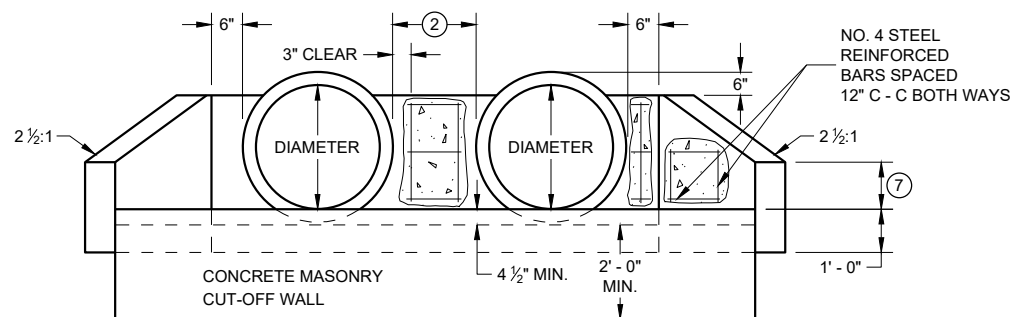
INLET			OUTLET		
R*	X	Y	R*	X	Y
0 - 7°	30°	30°	0 - 15°	15°	15°
8 - 22°	25°	30°	16 - 45°	10°	15°
23 - 37°	20°	30°	46 - 75°	5°	15°
38 - 52°	15°	30°	OVER 75°	0°	15°
53 - 67°	10°	30°			
68 - 82°	5°	30°			
OVER 82°	0°	30°			

R* = NUMBER OF DEGREES RIGHT OR LEFT HAND FORWARD

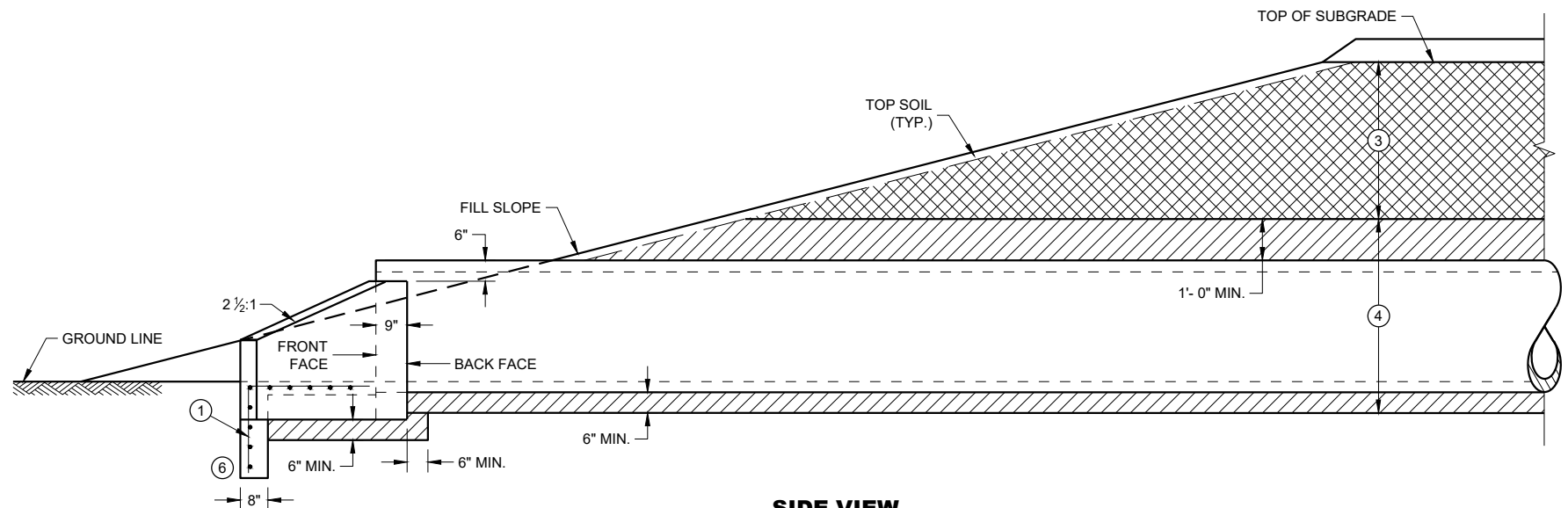
WINGWALL ANGLE DETAILS



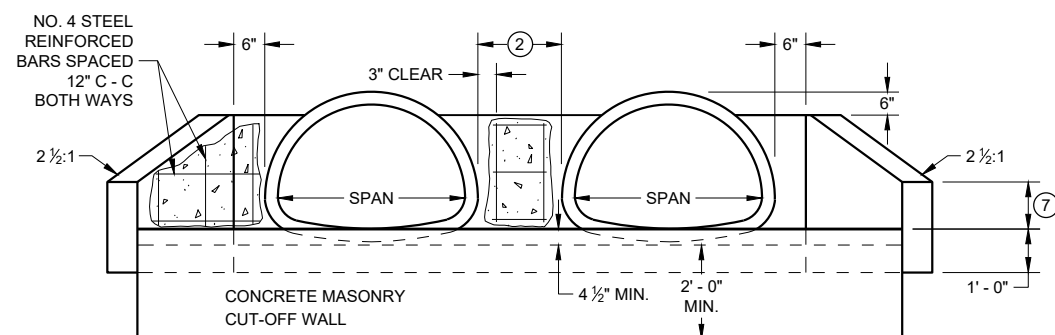
PLAN VIEW



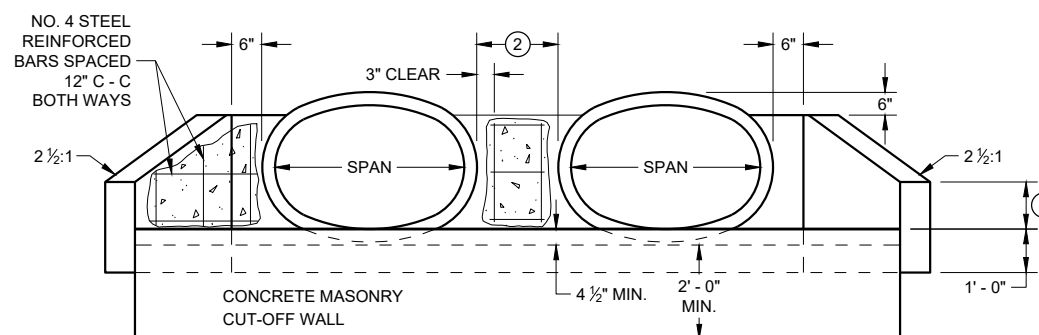
**END VIEW
CIRCULAR PIPE**



**SIDE VIEW
CIRCULAR PIPE, PIPE ARCH OR HORIZONTAL ELLIPSE**



**END VIEW
PIPE ARCH**



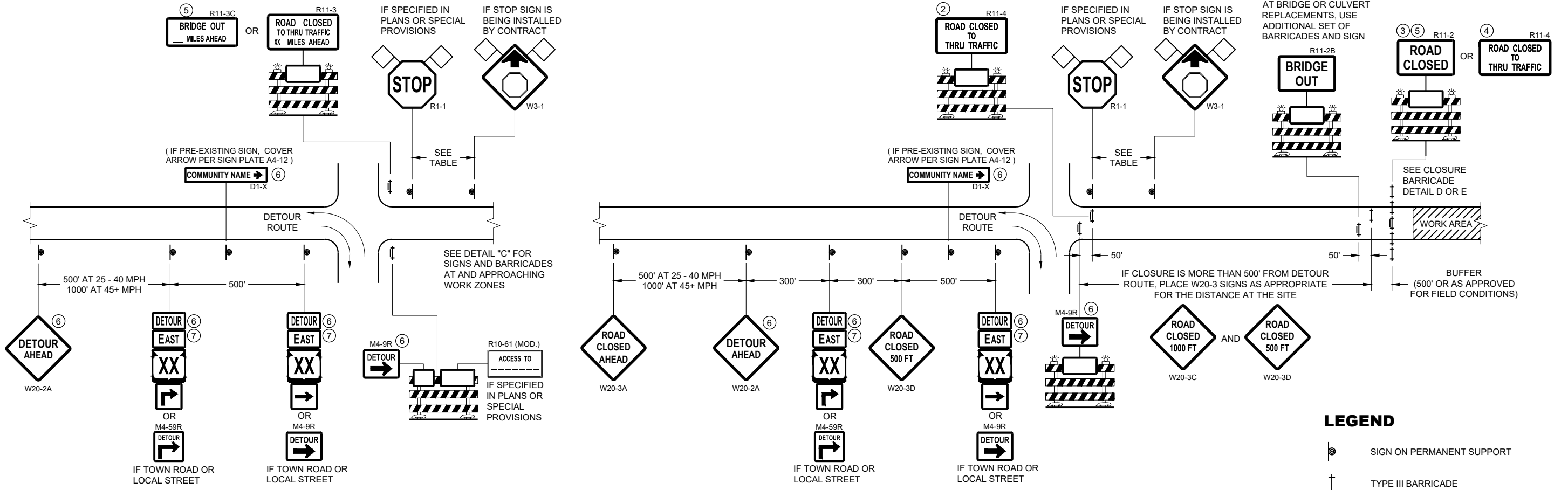
**END VIEW
HORIZONTAL ELLIPSE**

**CONCRETE MASONRY
ENDWALLS FOR CULVERT
PIPE AND PIPE ARCH**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2022 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
ENGINEER

FHWA



**DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR**

WORK ZONE GREATER THAN OR EQUAL TO 1/2 MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)

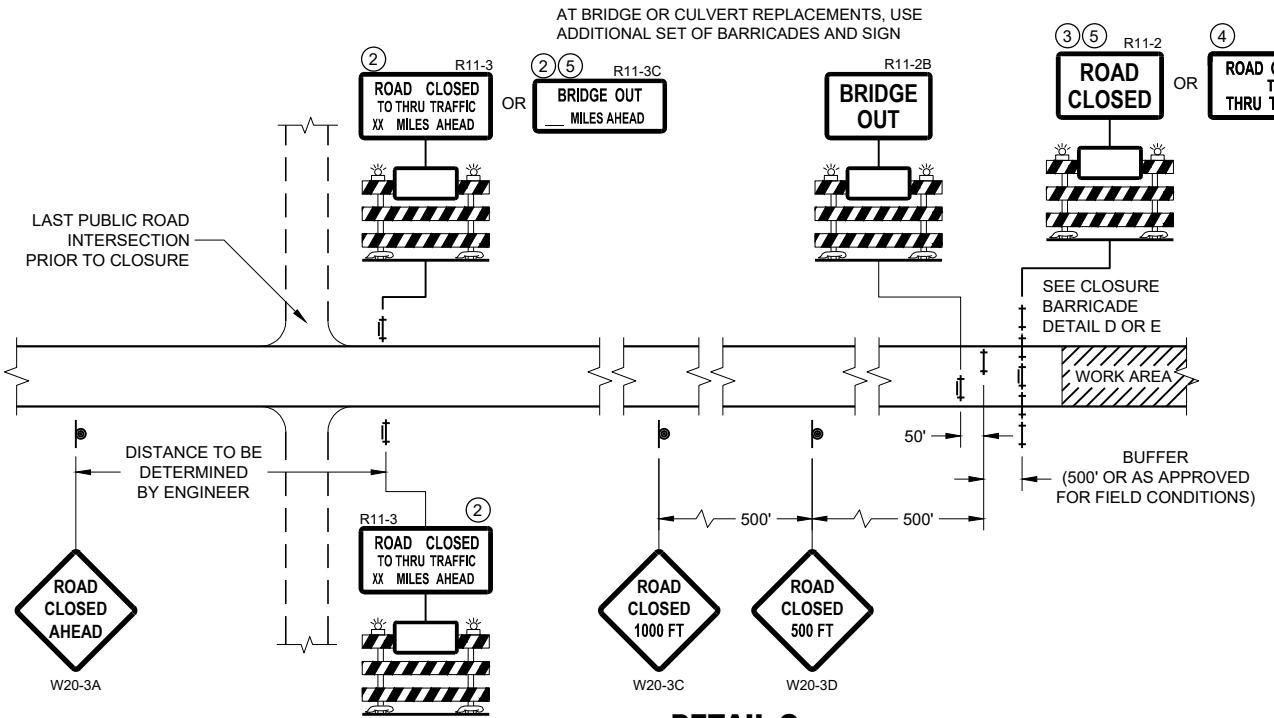
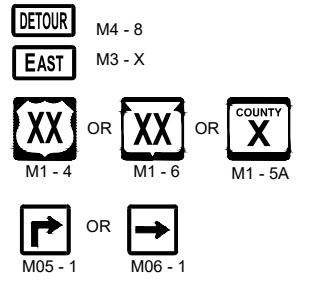
**DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR**

WORK ZONE LESS THAN 1/2 MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
- FLAGS, 16" X 16" MIN. (ORANGE)

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750



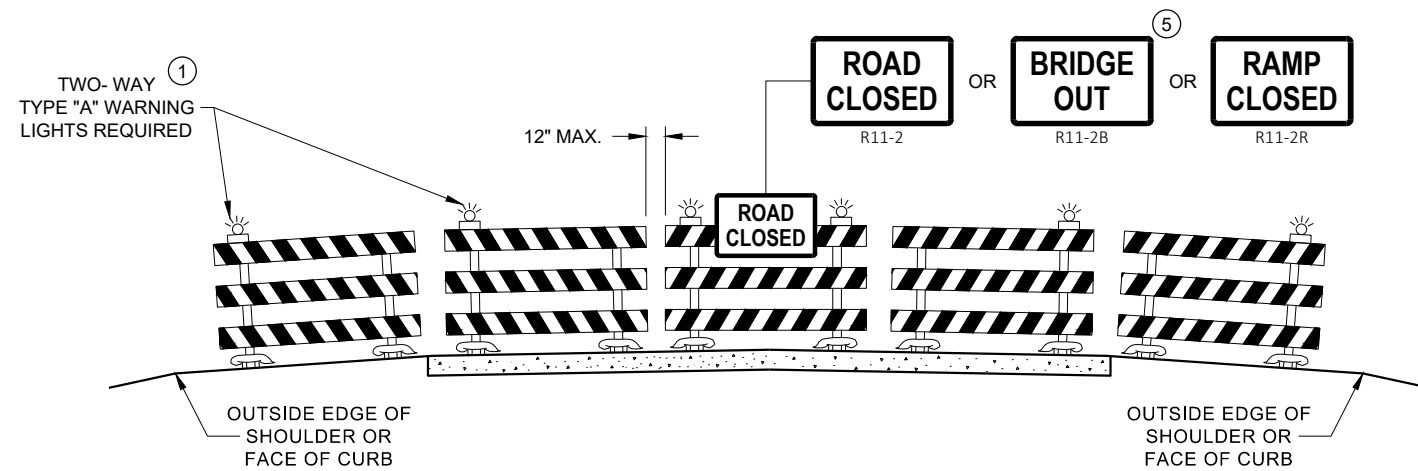
**DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR**

SEE SDD 15C2-SHEET "b"
FOR GENERAL NOTES
AND FOOTNOTES ① THROUGH ⑦

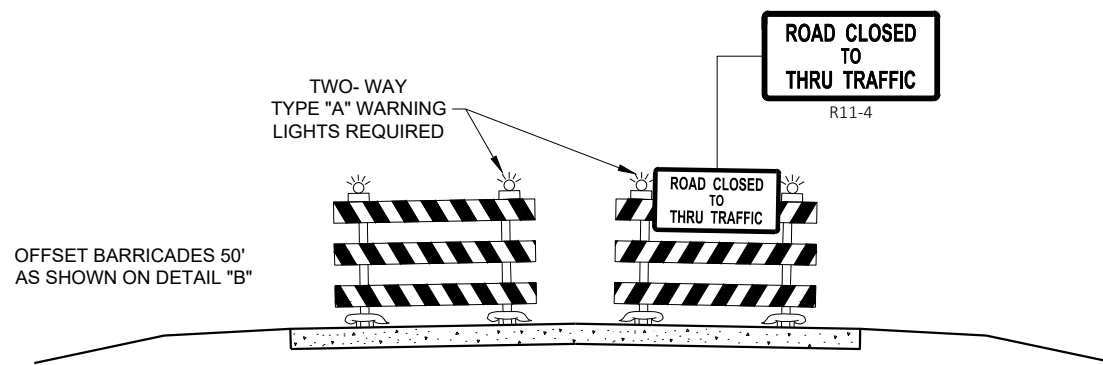
**BARRICADES AND SIGNS
FOR MAINLINE CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER



**DETAIL D
ROAD CLOSURE BARRICADE DETAIL
APPROACH VIEW**



**DETAIL E
LANE CLOSURE BARRICADE DETAIL
APPROACH VIEW**

SEE SDD 15C2 - SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW - INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11 - 2, R11 - 3, M4 - 9, R11 - 4, AND R10 - 61 SIGNS PLACED ON THE BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE RAIL OR BOTTOM RAILS.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

- R11 - 2 SHALL BE 48" X 30"
- R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60" X 30"
- M4 - 9 SHALL BE 30" X 24"
- M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1 - 1 SHALL BE 36" X 36"

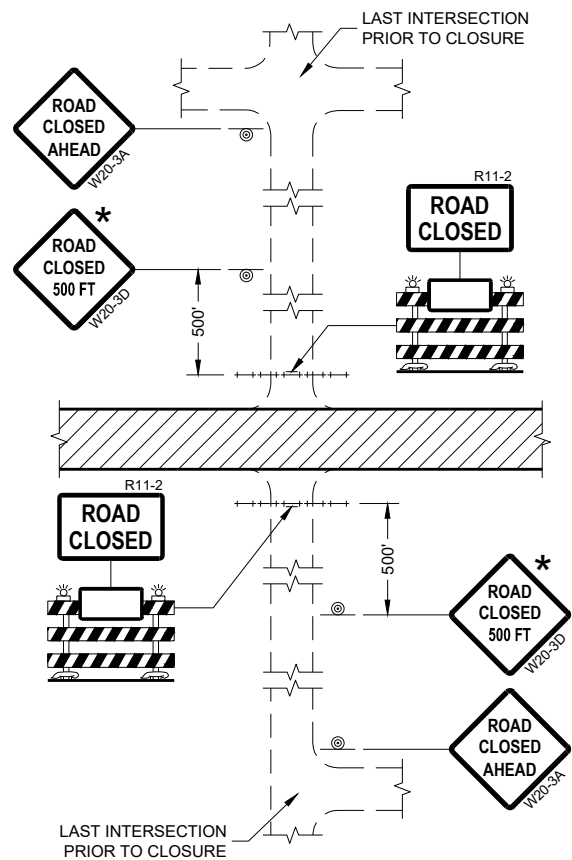
- ① TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT SPACING).
- ② THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.
- ③ FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".
- ④ FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".
- ⑤ FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.
- ⑥ INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.
- ⑦ "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

**BARRICADES AND SIGNS
FOR
VARIOUS CLOSURES**

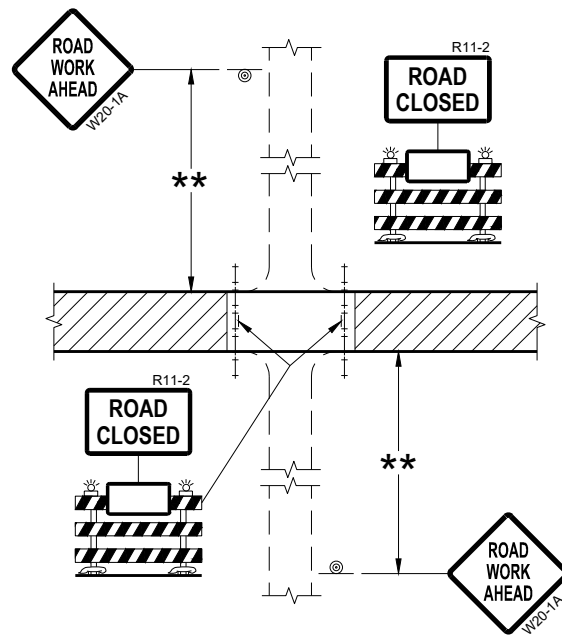
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

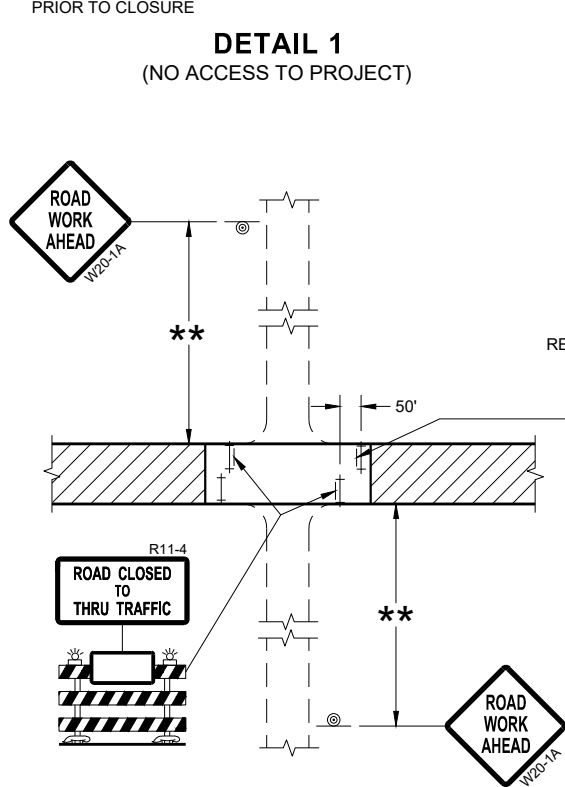
FHWA



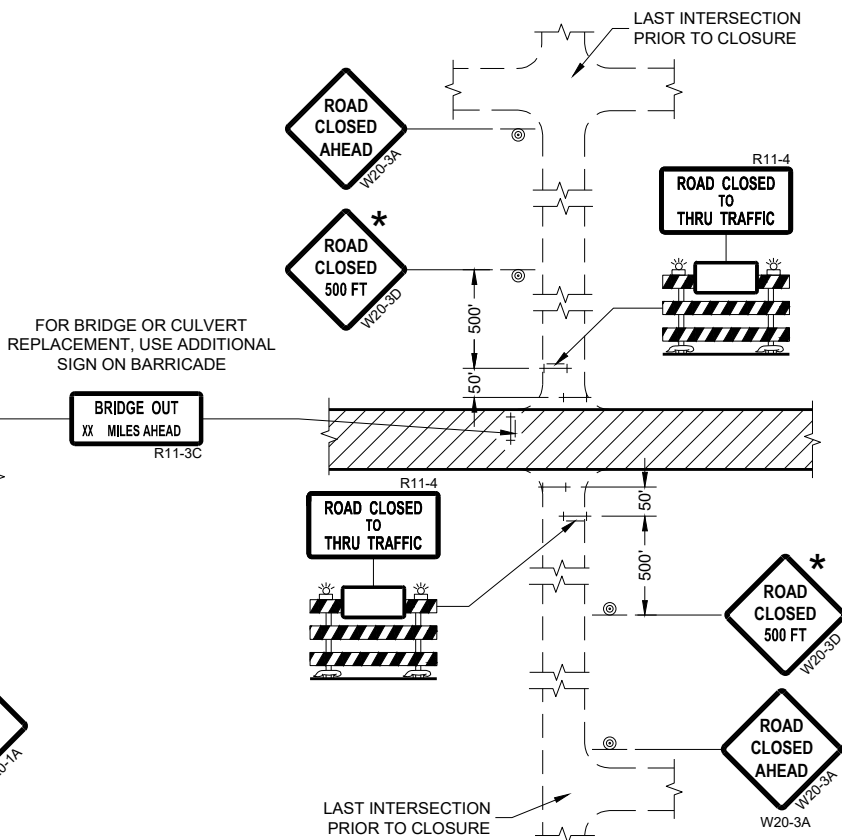
DETAIL 1
(NO ACCESS TO PROJECT)



DETAIL 2
(PUBLIC CROSS-TRAFFIC MAINTAINED.
NO ACCESS TO PROJECT)



DETAIL 3
(PUBLIC CROSS-TRAFFIC MAINTAINED.
CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)



DETAIL 4
(CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE (500 FEET DESIRABLE) TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3, AND R11-4 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
R11-2 SHALL BE 48" X 30".
R11-4 AND R11-3 SHALL BE 60" X 30".

- * OMIT THE "ROAD CLOSED 500 FT." SIGN IF THE LAST INTERSECTION IS 500 FEET OR LESS FROM THE WORK ZONE.
- ** 500' MAX. OR AT LAST INTERSECTION, WHICHEVER IS CLOSEST.

LEGEND

- ⊙ SIGN ON PERMANENT SUPPORT
- ⊥ TYPE III BARRICADE
- ⊥ TYPE III BARRICADE WITH ATTACHED SIGN
- ⚡ TYPE "A" WARNING LIGHT (FLASHING)
- ▨ WORK AREA

**BARRICADES AND SIGNS
FOR
SIDEROAD CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION




APPROVED
July 2018 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

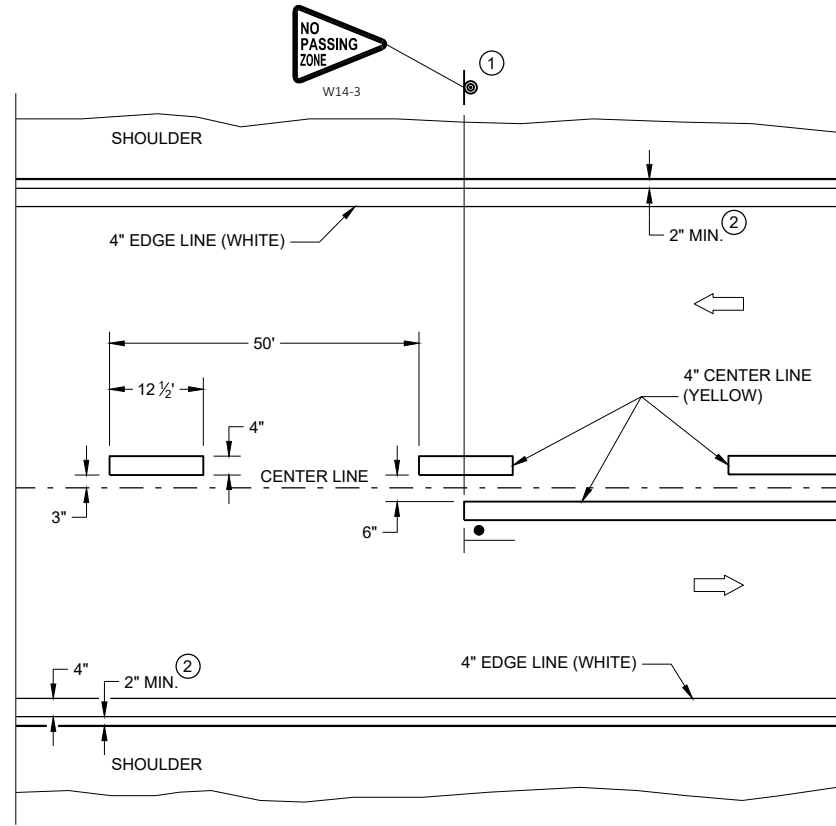
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

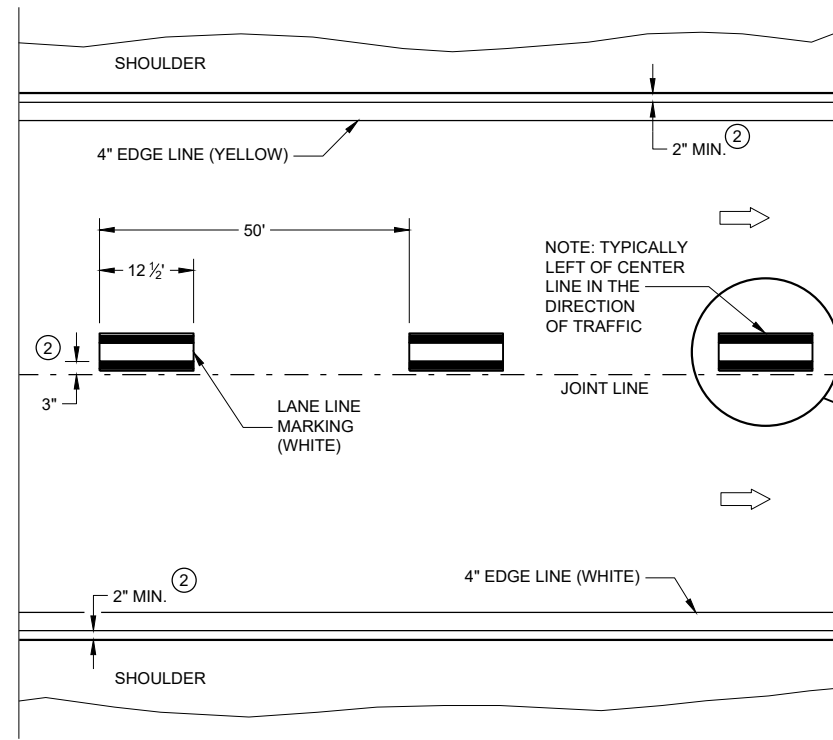
- ① LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING
- ② MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

LEGEND

-  "T" MARKING
-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC

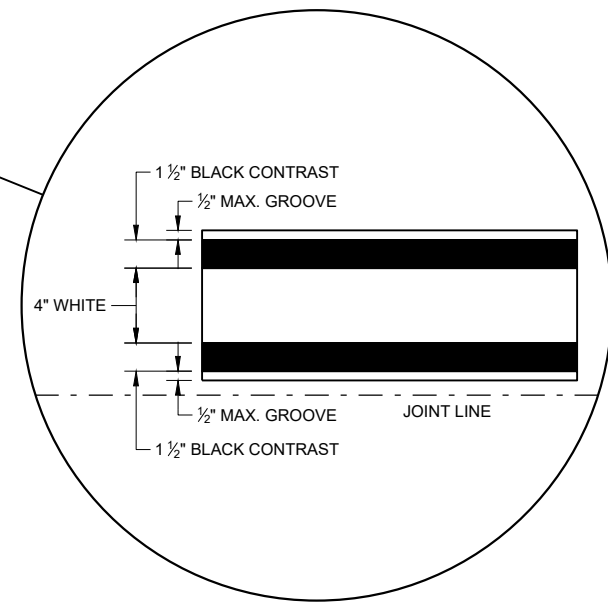


TWO WAY TRAFFIC



ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING



PERMANENT LONGITUDINAL PAVEMENT MARKINGS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE: May 2022 /S/ Jeannie Silver
STATEWIDE SIGNING AND MARKING ENGINEER

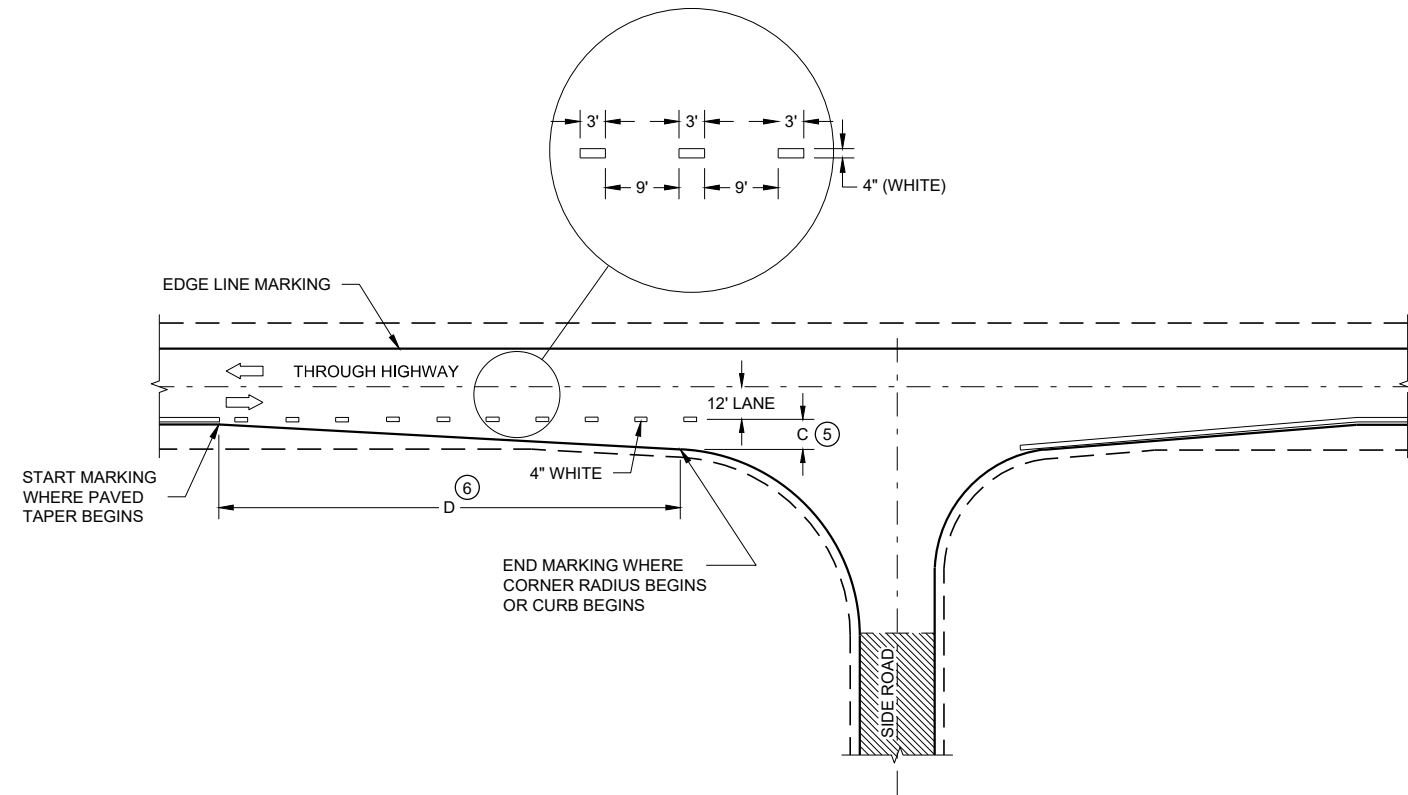
GENERAL NOTES

OMIT EDGE LINES THROUGH INTERSECTIONS. CONTINUE EDGE LINES THROUGH DRIVEWAYS.

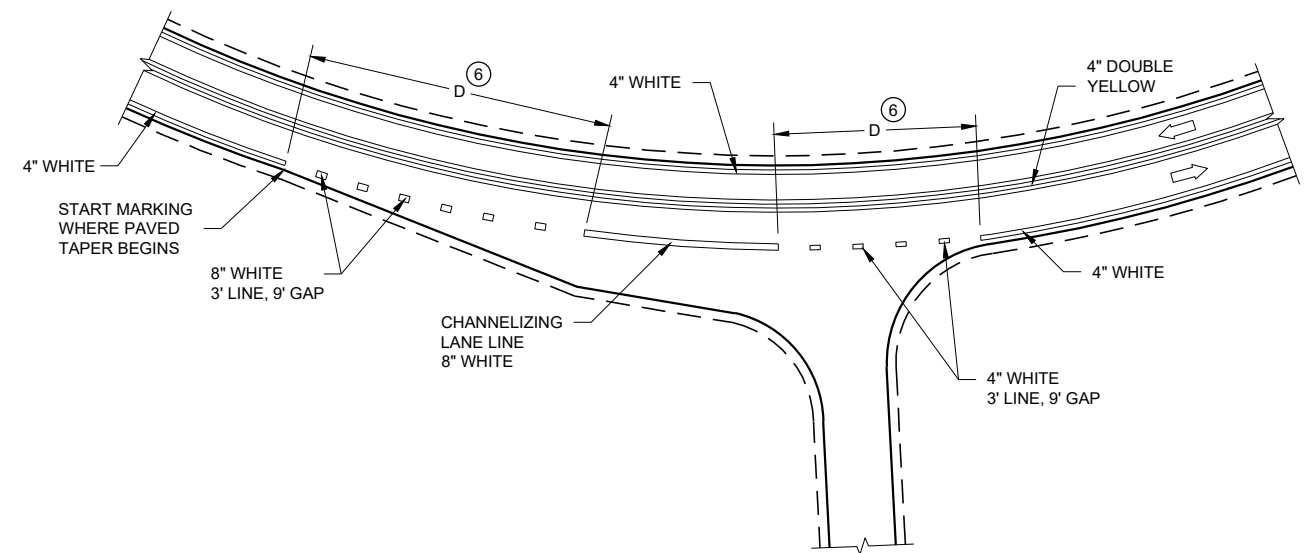
- ① WHEN DISTANCE "A" IS LESS THAN 250 FEET, OMIT LANE LINE.
- ② WHEN DISTANCE "B" IS LESS THAN 100 FEET, OMIT CHANNELIZING LANE LINE.
- ③ BARRIER LINE ENDS AT SIDE ROAD PAVEMENT / SURFACE EDGE EXTENSION.
- ④ BARRIER LINE STARTS 500 FEET PRIOR TO THE BYPASS TAPER.
- ⑤ WHEN DISTANCE "C" IS LESS THAN 4 FEET, OMIT DOTTED EXTENSION.
- ⑥ WHEN DISTANCE "D" IS LESS THAN 50 FEET, OMIT DOTTED EXTENSION.

LEGEND

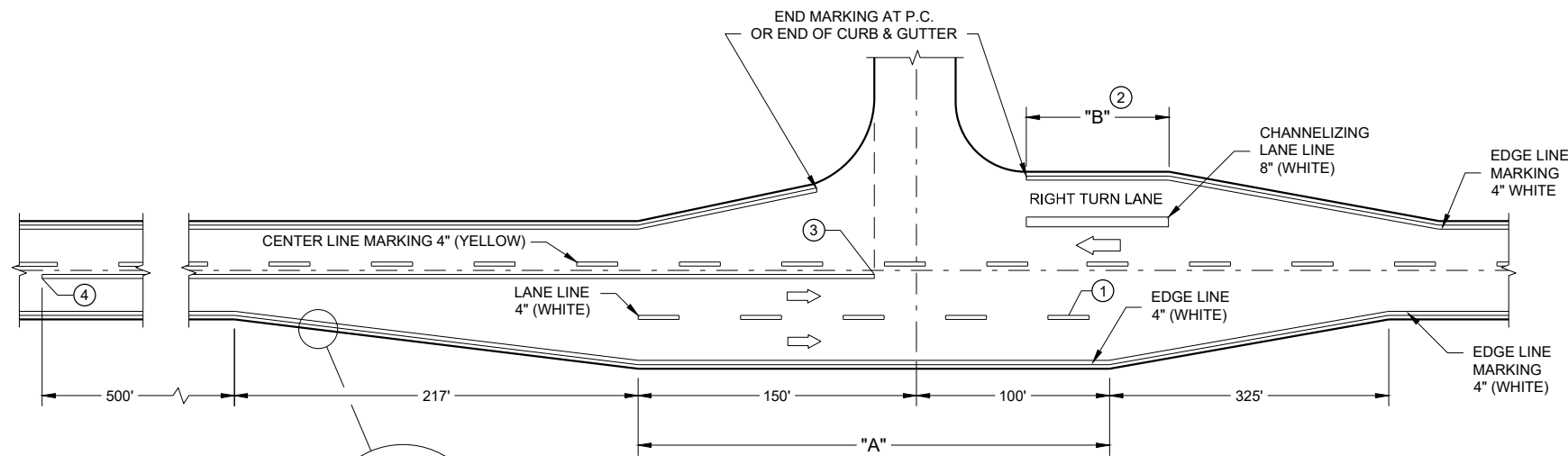
➔ DIRECTION OF TRAVEL



MINOR INTERSECTION

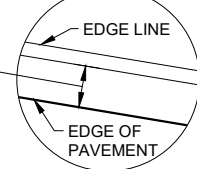


INTERSECTION ON OUTSIDE OF CURVE



**MAJOR INTERSECTIONS
(INTERSECTION WITH FULL RIGHT TURN LANE OR BYPASS LANE)**

BYPASS LANE PAVED SHOULDER WIDTH (AS SHOWN ELSEWHERE IN PLANS) - PLUS 2 INCHES



**PAVEMENT MARKING
(INTERSECTIONS)**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

DIVISION 1 - S_HASTINGS_WAY_PR

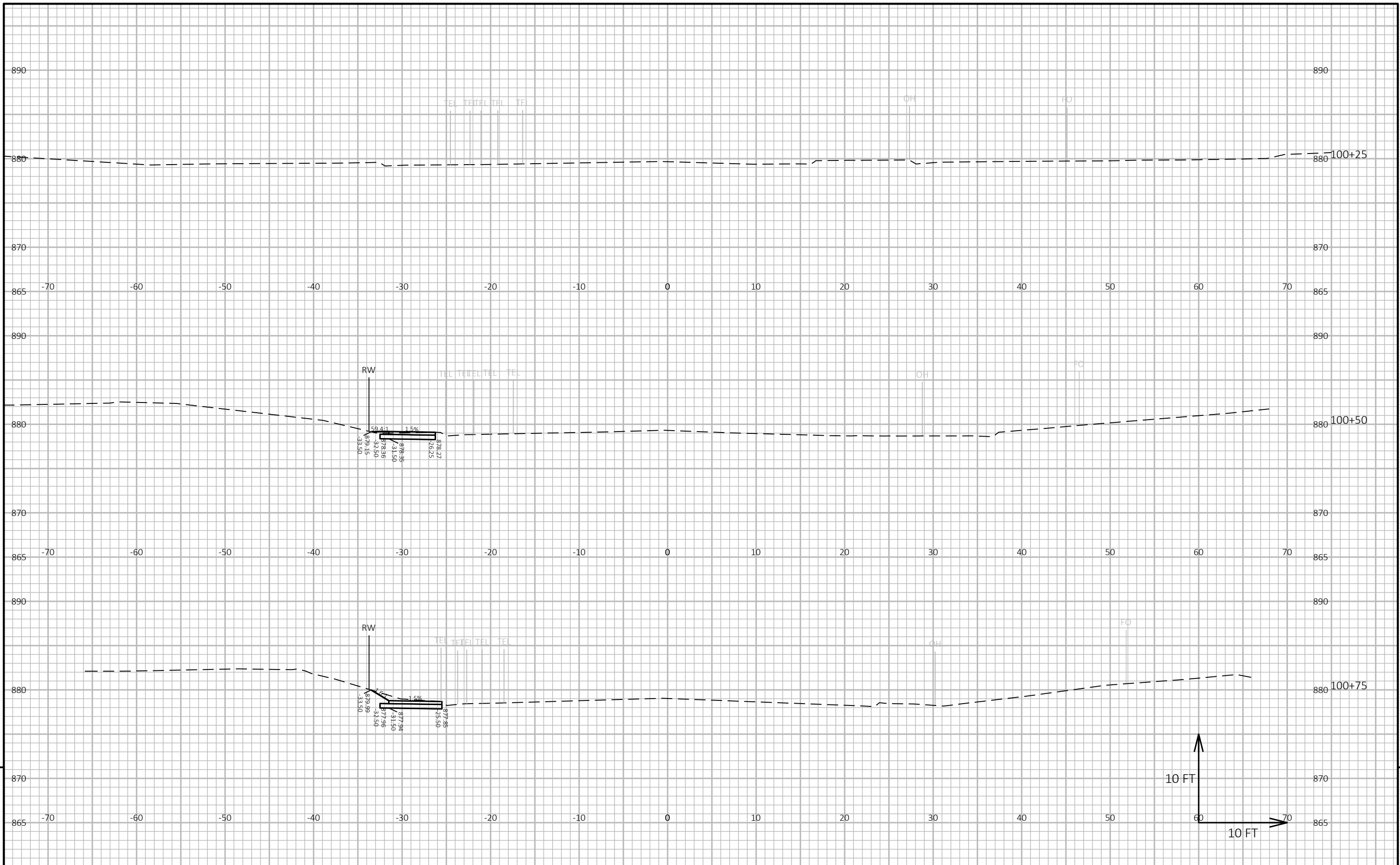
STATION	DISTANCE	AREA (SF)			INCREMENTAL VOL (CY) (UNADJUSTED)			CUMULATIVE VOL (CY)		
		CUT	SALVAGED/UNUSABLE PAVEMENT MATERIAL	FILL	CUT	SALVAGED/UNUSABLE PAVEMENT MATERIAL	FILL	CUT	EXPANDED FILL	MASS ORDINATE
100+25	0.00	0.00	0.00	0.00	0	0	0	0	0	0
100+50	25.00	4.51	0.00	0.12	2	0	0	2	0	2
100+75	25.00	7.79	0.00	0.00	6	0	0	8	0	8
101+00	25.00	53.24	10.00	1.67	28	5	1	36	1	30
101+50	50.00	56.23	10.00	2.01	101	19	3	137	5	108
102+00	50.00	45.48	10.00	4.93	94	19	6	231	12	176
102+50	50.00	52.86	10.00	2.64	91	19	7	322	20	240
103+00	50.00	51.52	10.00	3.31	97	19	6	419	28	310
103+05	5.00	54.04	10.00	2.17	10	2	1	429	29	317
103+50	45.00	38.18	10.00	9.57	77	17	10	506	41	365
104+00	50.00	40.67	10.00	8.55	73	19	17	579	61	399
104+50	50.00	36.37	10.00	10.85	71	19	18	650	83	429
105+00	50.00	31.22	10.00	14.74	63	19	24	713	112	444
105+50	50.00	31.48	10.00	15.72	58	19	28	771	145	450
106+00	50.00	33.50	10.00	12.27	60	19	26	831	176	460
106+35	35.00	52.10	10.00	7.36	55	13	13	886	192	486
106+50	15.00	48.28	10.00	6.88	28	6	4	914	197	503
107+00	50.00	57.79	10.00	3.62	98	19	10	1,012	209	570
107+50	50.00	83.82	10.00	0.43	131	19	4	1,143	214	677
108+00	50.00	101.81	10.00	0.00	172	19	0	1,315	214	830
108+50	50.00	113.43	10.00	0.00	199	19	0	1,514	214	1,010
109+00	50.00	109.05	10.00	0.00	206	19	0	1,720	214	1,197
109+50	50.00	89.90	10.00	0.60	184	19	1	1,904	215	1,361
110+00	50.00	73.48	10.00	4.27	151	19	5	2,055	221	1,487
110+50	50.00	72.86	10.00	4.31	136	19	8	2,191	230	1,595
110+80	30.00	106.48	10.00	0.00	100	11	2	2,291	233	1,681
111+00	20.00	89.62	10.00	0.20	73	7	0	2,364	233	1,717
111+50	50.00	85.37	10.00	3.85	162	19	4	2,526	238	1,885
112+00	50.00	90.72	10.00	4.40	163	19	8	2,689	247	2,020
112+50	50.00	100.06	10.00	2.90	177	19	7	2,866	256	2,169
113+00	50.00	110.57	10.00	0.57	195	19	3	3,061	259	2,342
113+50	50.00	122.18	10.00	1.94	216	19	2	3,277	262	2,536
114+00	50.00	124.12	10.00	0.63	228	19	2	3,505	264	2,743
114+50	50.00	135.02	10.00	1.26	240	19	2	3,745	266	2,962
115+00	50.00	143.27	10.00	0.65	258	19	2	4,003	269	3,198
115+50	50.00	155.93	10.00	0.22	277	19	1	4,280	270	3,455
116+00	50.00	167.56	10.00	0.00	300	19	0	4,580	270	3,736
116+50	50.00	169.28	10.00	0.00	312	19	0	4,892	270	4,029
117+00	50.00	158.17	10.00	0.00	303	19	0	5,195	270	4,313
117+50	50.00	141.11	10.00	0.00	277	19	0	5,472	270	4,571
118+00	50.00	124.87	10.00	0.03	246	19	0	5,718	270	4,798

STATION	DISTANCE	AREA (SF)			INCREMENTAL VOL (CY) (UNADJUSTED)			CUMULATIVE VOL (CY)		
		CUT	SALVAGED/UNUSABLE PAVEMENT MATERIAL	FILL	CUT	SALVAGED/UNUSABLE PAVEMENT MATERIAL	FILL	CUT	EXPANDED FILL	MASS ORDINATE
118+50	50.00	98.47	10.00	7.90	207	19	3	5,925	274	4,982
119+00	50.00	136.94	10.00	0.00	218	19	3	6,143	277	5,178
119+50	50.00	86.28	10.00	22.30	207	19	21	6,350	302	5,341
120+00	50.00	98.69	10.00	6.38	171	19	27	6,521	335	5,460
120+50	50.00	106.90	10.00	13.38	190	19	18	6,711	356	5,610
121+00	50.00	113.36	10.00	14.23	204	19	26	6,915	388	5,763
121+50	50.00	130.19	10.00	21.00	226	19	33	7,141	427	5,931
122+00	50.00	116.61	10.00	8.47	229	19	27	7,370	460	6,108
122+50	50.00	139.95	10.00	2.80	238	19	10	7,608	472	6,315
123+00	50.00	150.14	10.00	0.52	269	19	3	7,877	475	6,562
123+50	50.00	149.25	10.00	1.02	277	19	1	8,154	476	6,819
124+00	50.00	143.56	10.00	2.55	271	19	3	8,425	480	7,067
124+50	50.00	136.87	10.00	8.21	260	19	10	8,685	492	7,296
125+00	50.00	130.17	10.00	16.10	247	19	23	8,932	520	7,496
125+50	50.00	131.79	10.00	20.08	243	19	34	9,175	560	7,680
126+00	50.00	129.68	10.00	24.23	242	19	41	9,417	610	7,853
126+50	50.00	125.67	10.00	22.84	236	19	44	9,653	662	8,018
127+00	50.00	121.12	10.00	22.08	229	19	42	9,882	713	8,177
127+50	50.00	111.46	10.00	25.22	215	19	44	10,097	766	8,320
128+00	50.00	104.28	10.00	35.50	200	19	56	10,297	833	8,434
128+50	50.00	100.78	10.00	51.82	190	19	81	10,487	930	8,508
129+00	50.00	94.47	10.00	54.92	181	19	99	10,668	1,049	8,551
129+50	50.00	86.53	10.00	57.82	168	19	104	10,836	1,174	8,575
130+00	50.00	77.79	10.00	69.94	152	19	118	10,988	1,315	8,567
130+50	50.00	65.44	10.00	58.05	133	19	119	11,121	1,458	8,538
131+00	50.00	52.92	10.00	77.51	110	19	126	11,231	1,609	8,478
131+50	50.00	46.49	10.00	103.05	92	19	167	11,323	1,810	8,350
132+00	50.00	41.74	10.00	110.05	82	19	197	11,405	2,046	8,177
132+50	50.00	42.09	10.00	99.97	78	19	194	11,483	2,279	8,003
133+00	50.00	53.78	10.00	88.88	89	19	175	11,572	2,489	7,863

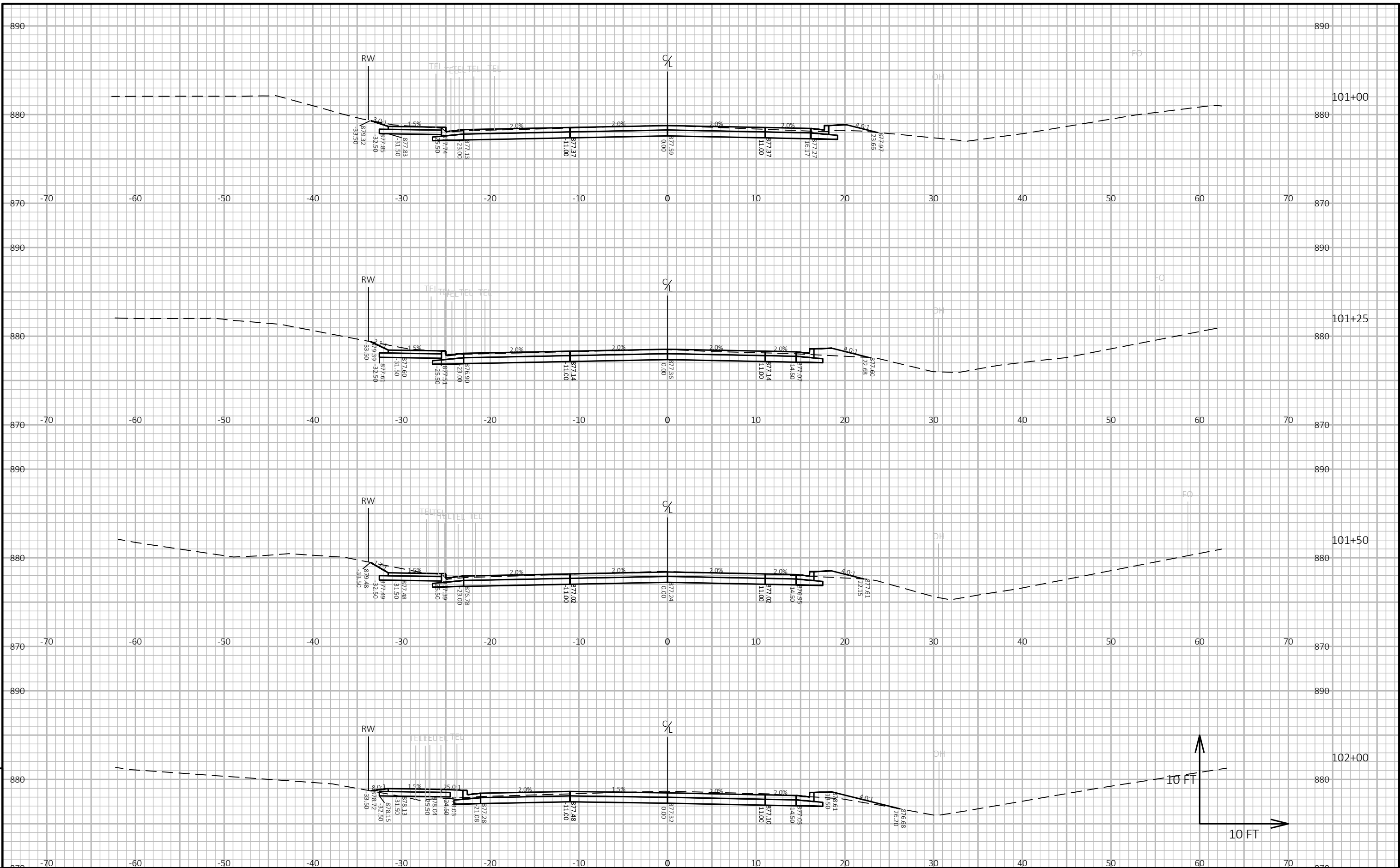
Notes:	
1 - CUT	CUT INCLUDES SALVAGED/UNUSABLE PAVEMENT MATERIAL
2 - SALVAGED/UNUSABLE PAVEMENT MATERIAL	THIS DOES NOT SHOW UP IN CROSS SECTIONS
3 - FILL	DOES NOT INCLUDE UNUSABLE PAVEMENT EXC VOLUME
4 - MASS ORDINATE	[(CUT - SALVAGED PAVT) - (FILL * FILL FACTOR)]

9

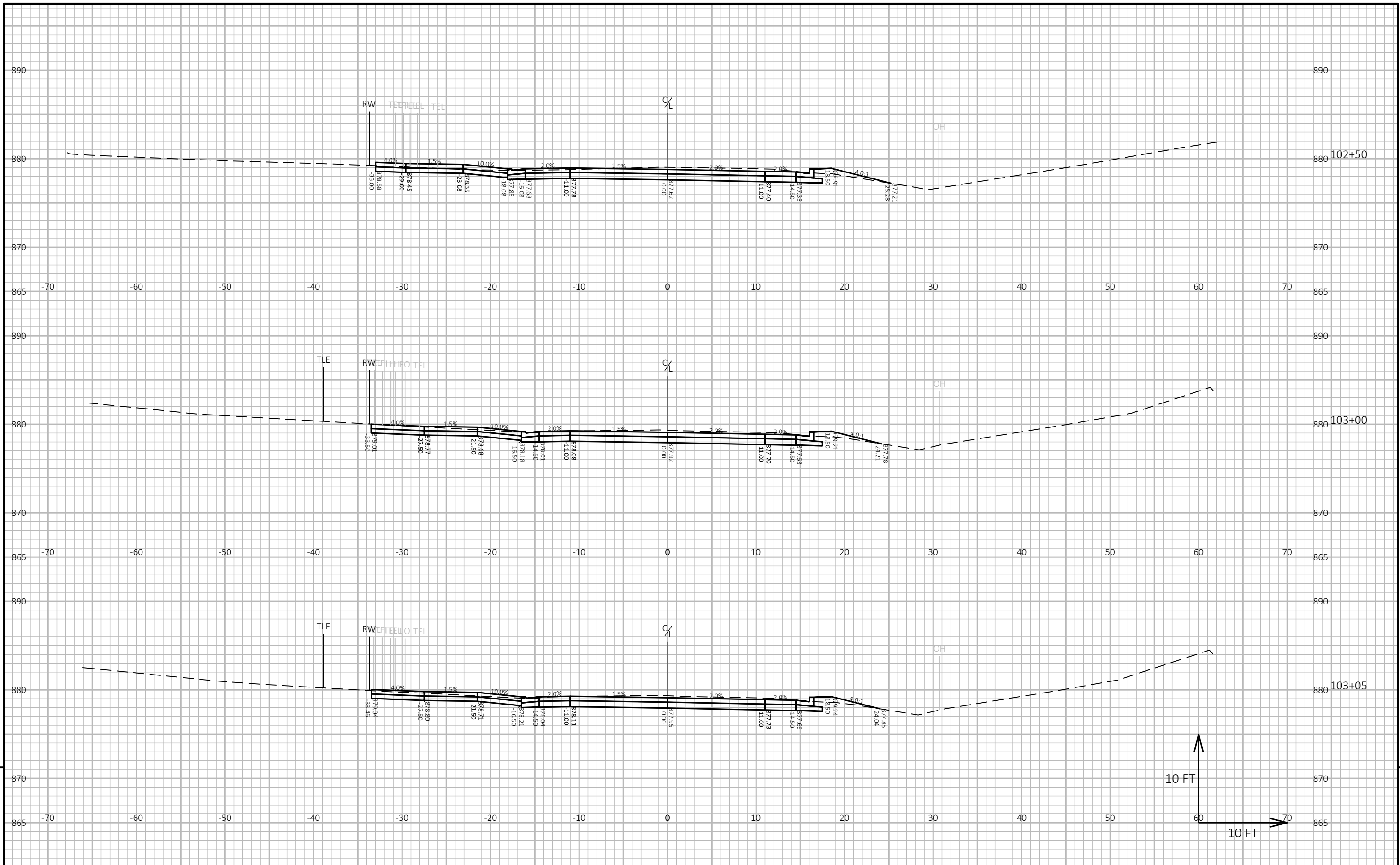
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PROJECT NO: 7995-02-65 HWY: S HASTINGS WAY COUNTY: EAU CLAIRE CROSS SECTIONS: S HASTINGS WAY SHEET E



PROJECT NO: 7995-02-65 HWY: S HASTINGS WAY COUNTY: EAU CLAIRE CROSS SECTIONS: S HASTINGS WAY SHEET E



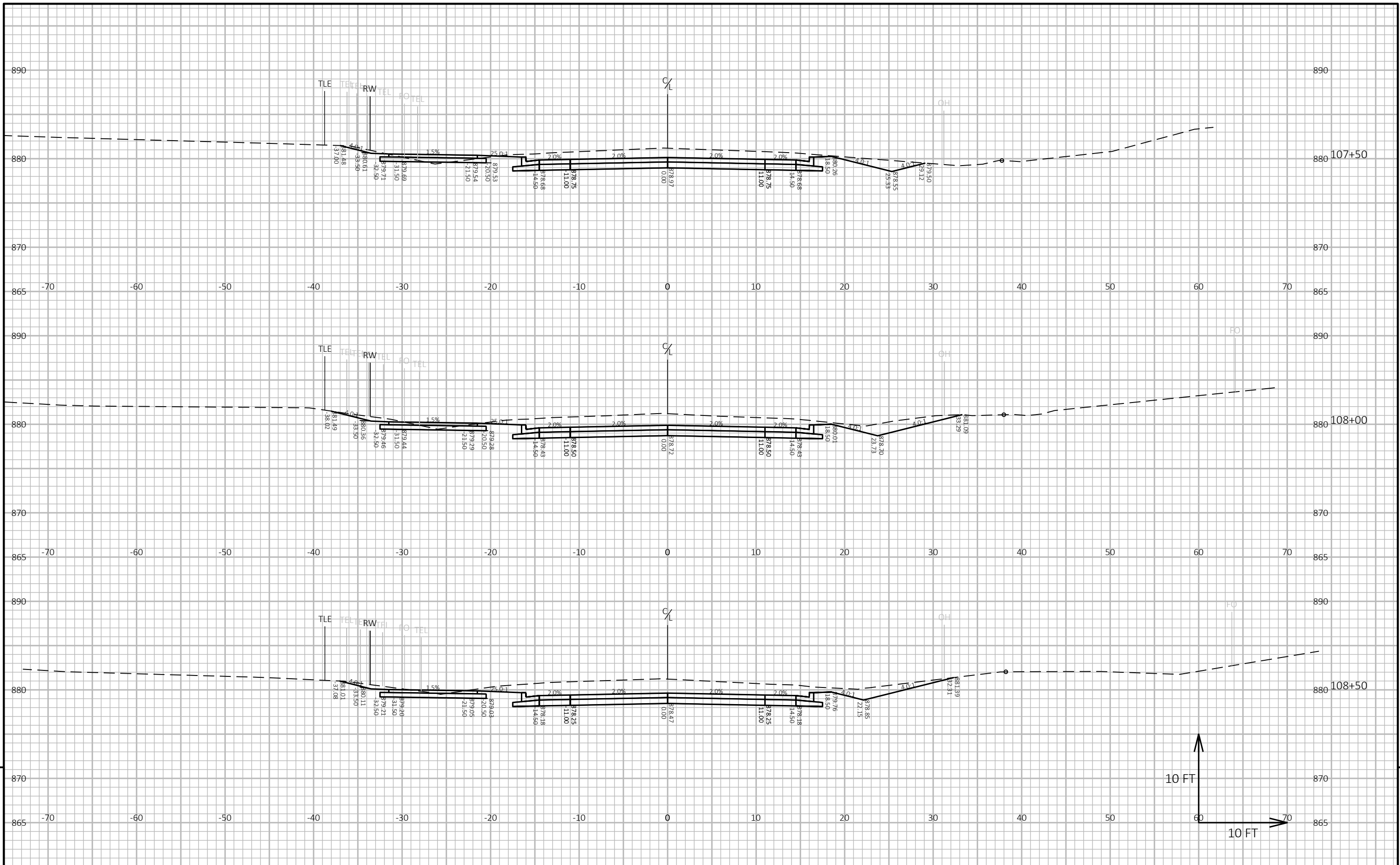
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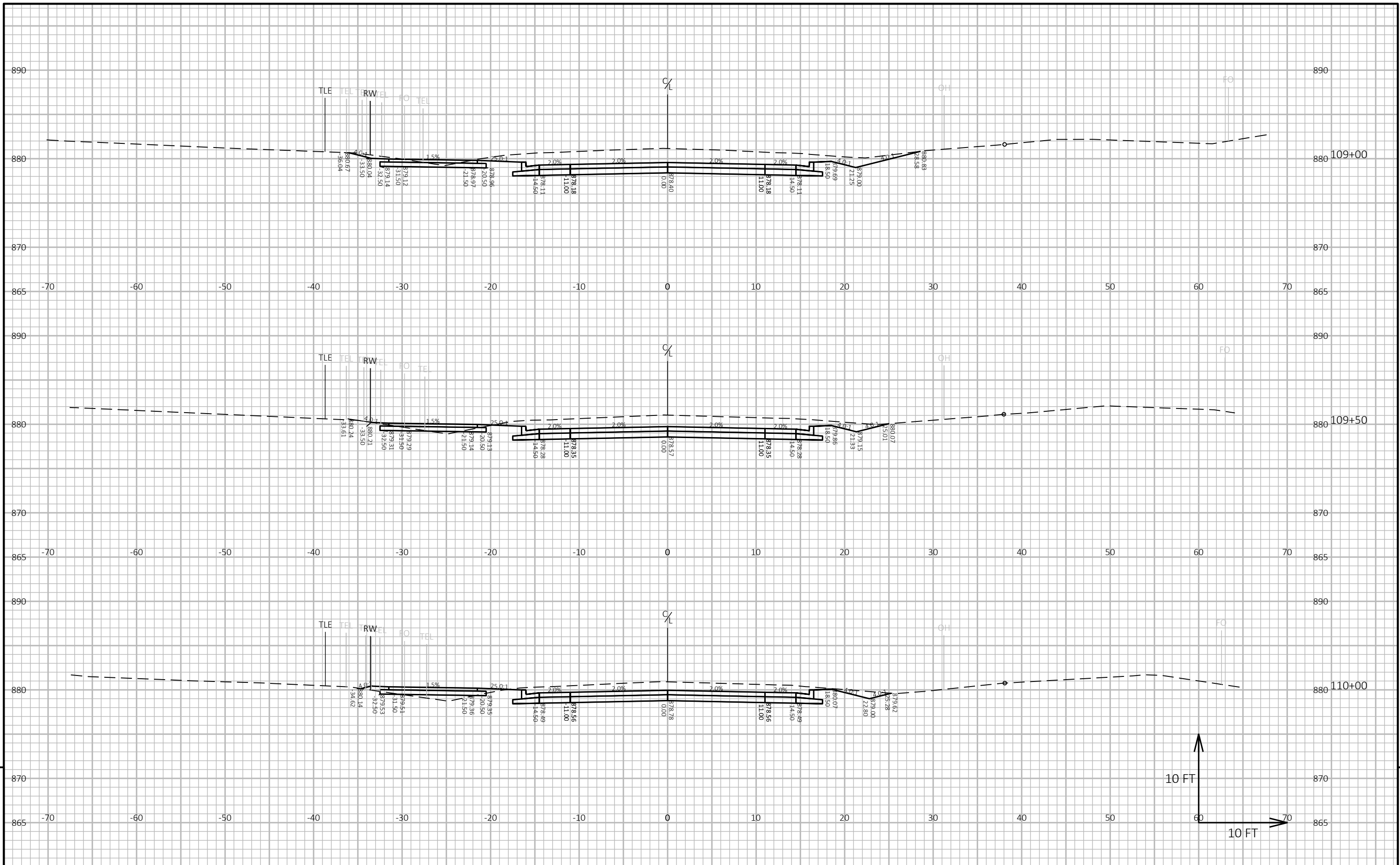
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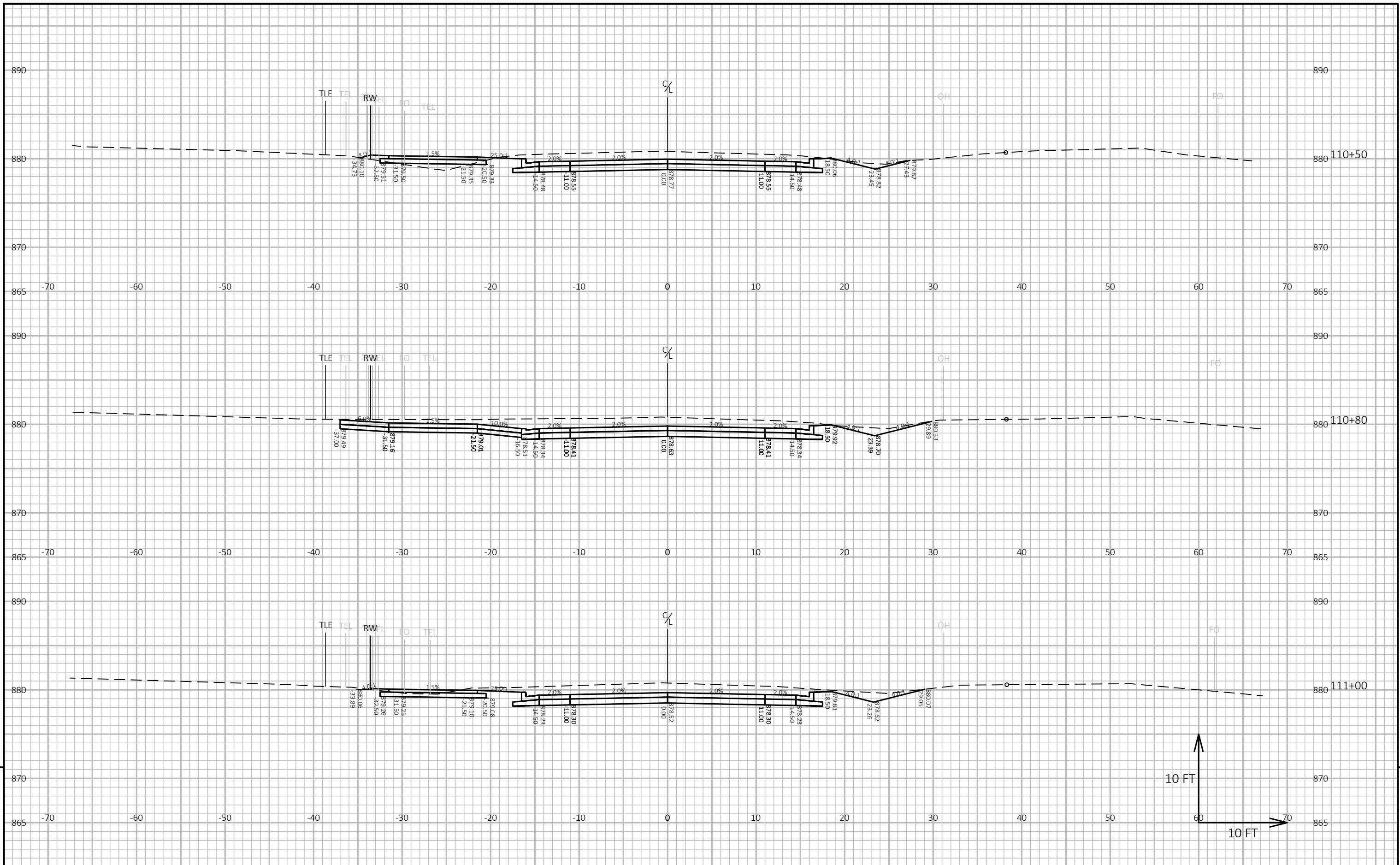
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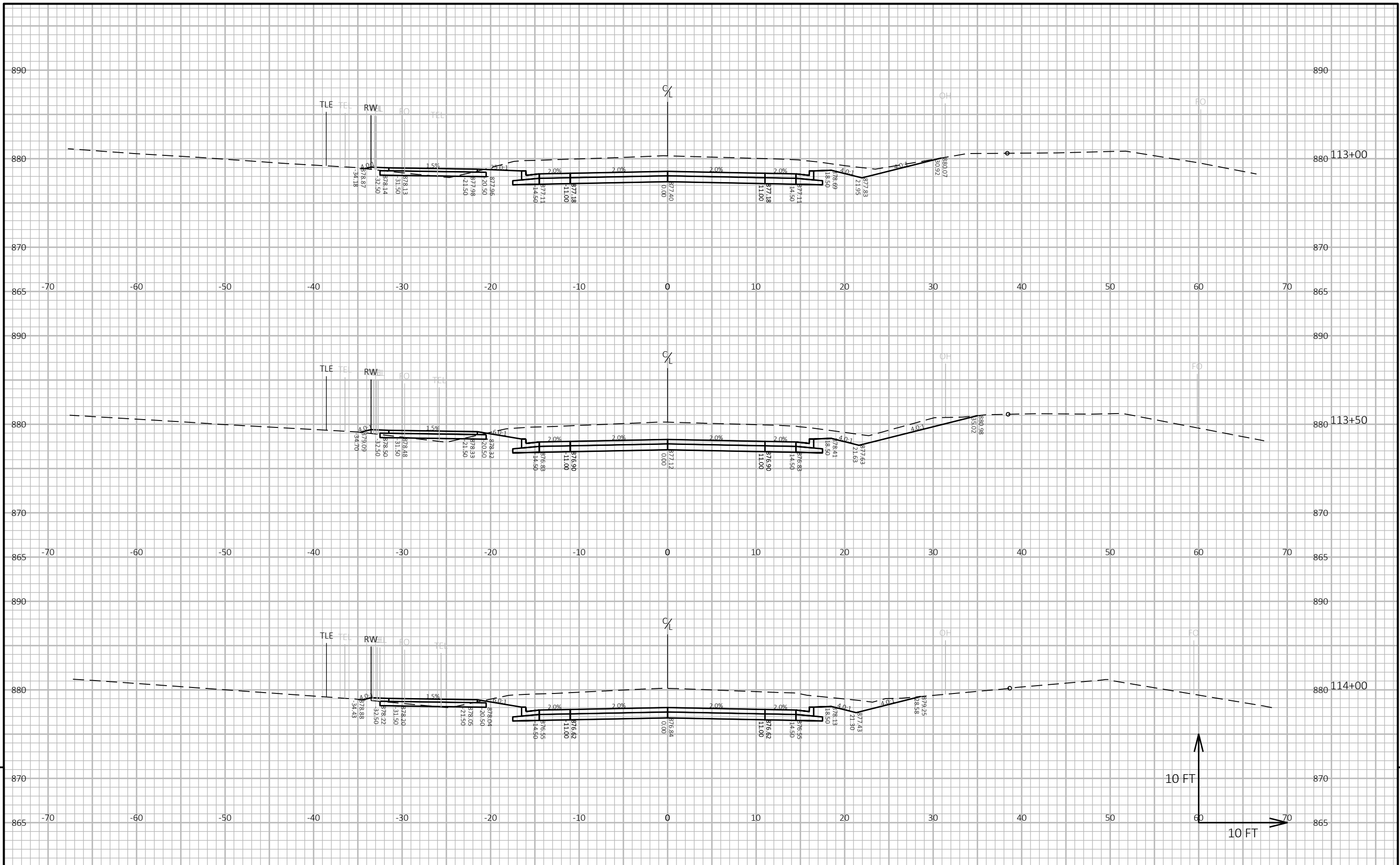
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PROJECT NO: 7995-02-65 HWY: S HASTINGS WAY COUNTY: EAU CLAIRE CROSS SECTIONS: S HASTINGS WAY SHEET E



PROJECT NO: 7995-02-65 HWY: S HASTINGS WAY COUNTY: EAU CLAIRE CROSS SECTIONS: S HASTINGS WAY SHEET E

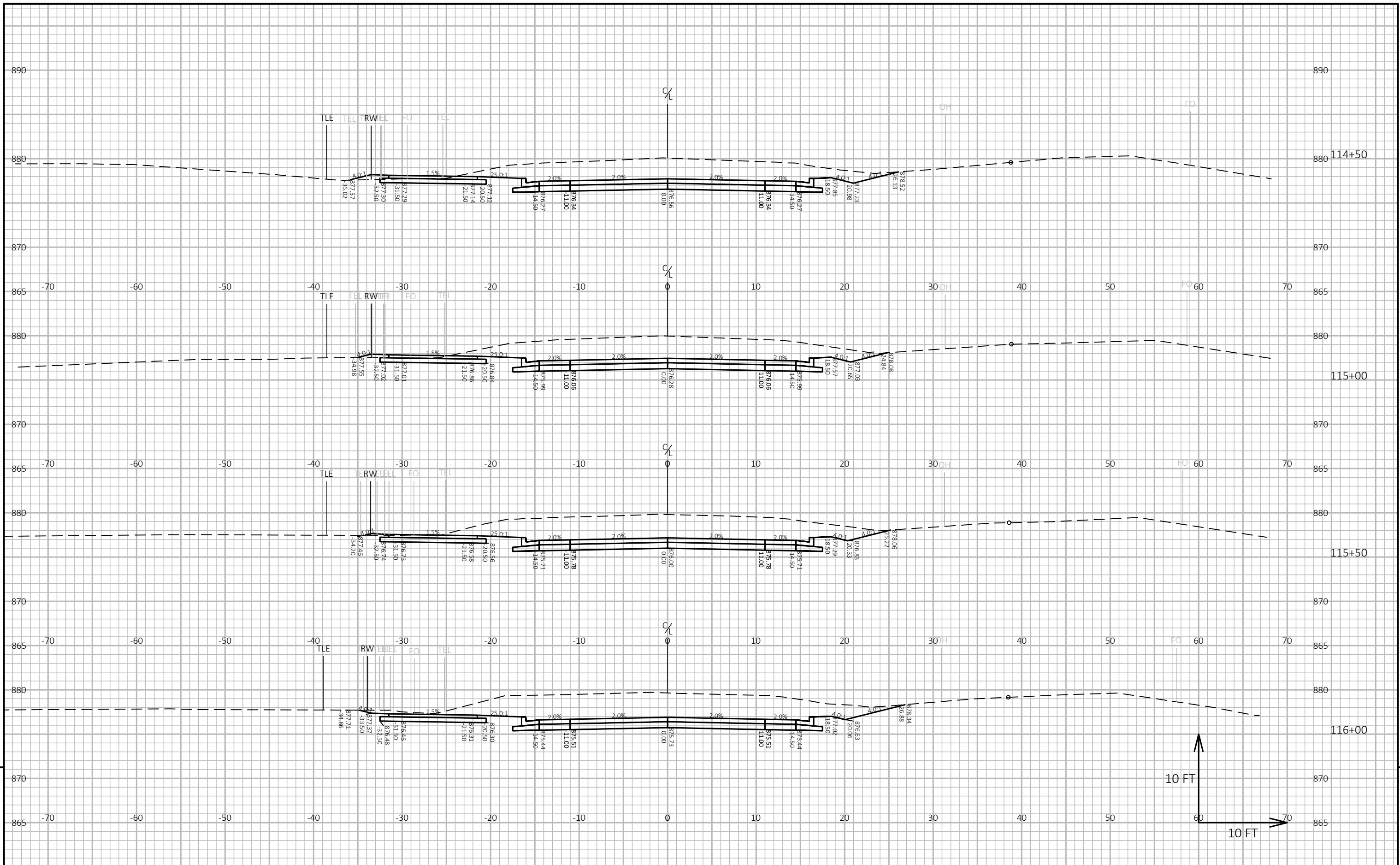


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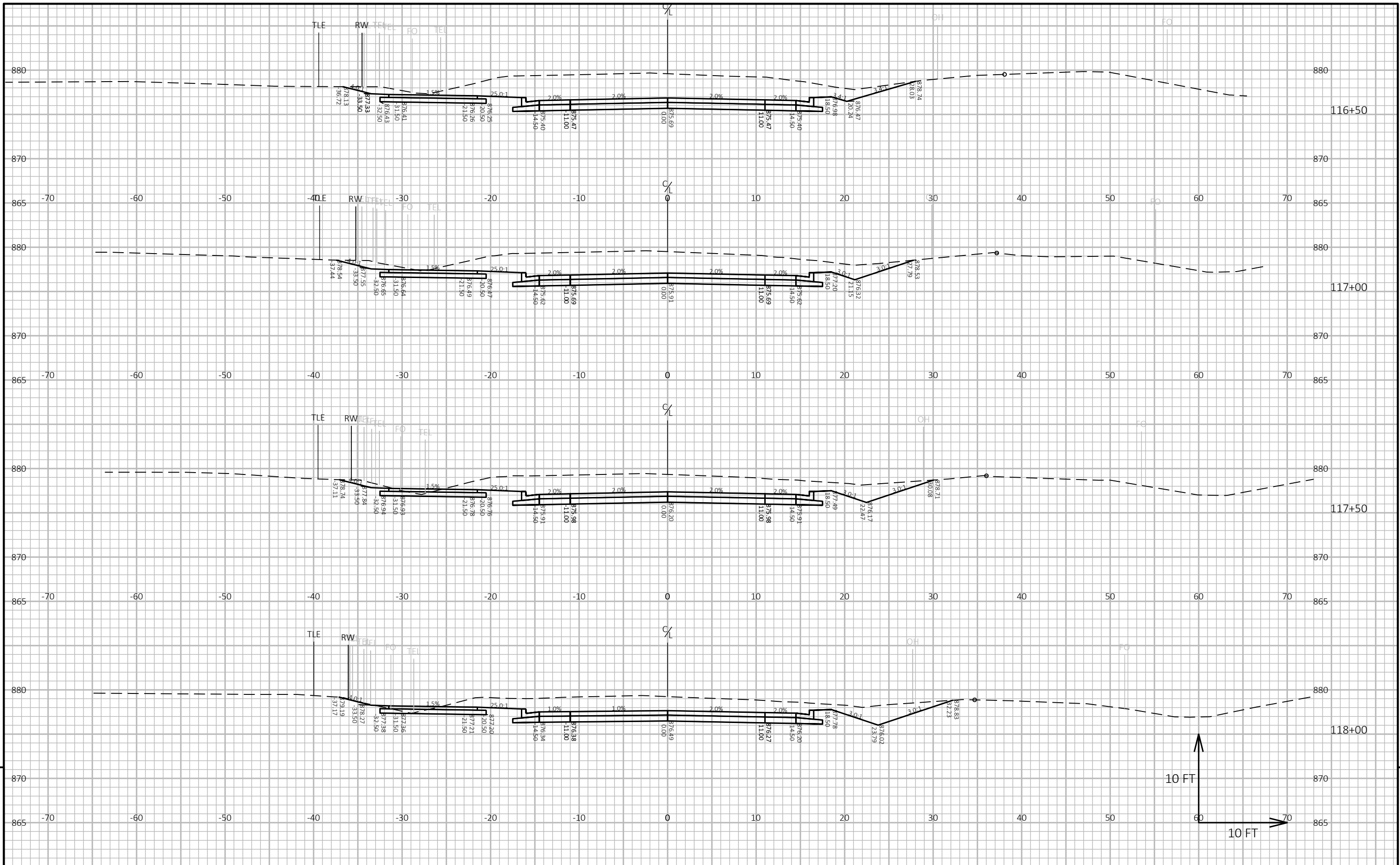
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PROJECT NO: 7995-02-65 HWY: S HASTINGS WAY COUNTY: EAU CLAIRE CROSS SECTIONS: S HASTINGS WAY SHEET E



PROJECT NO: 7995-02-65

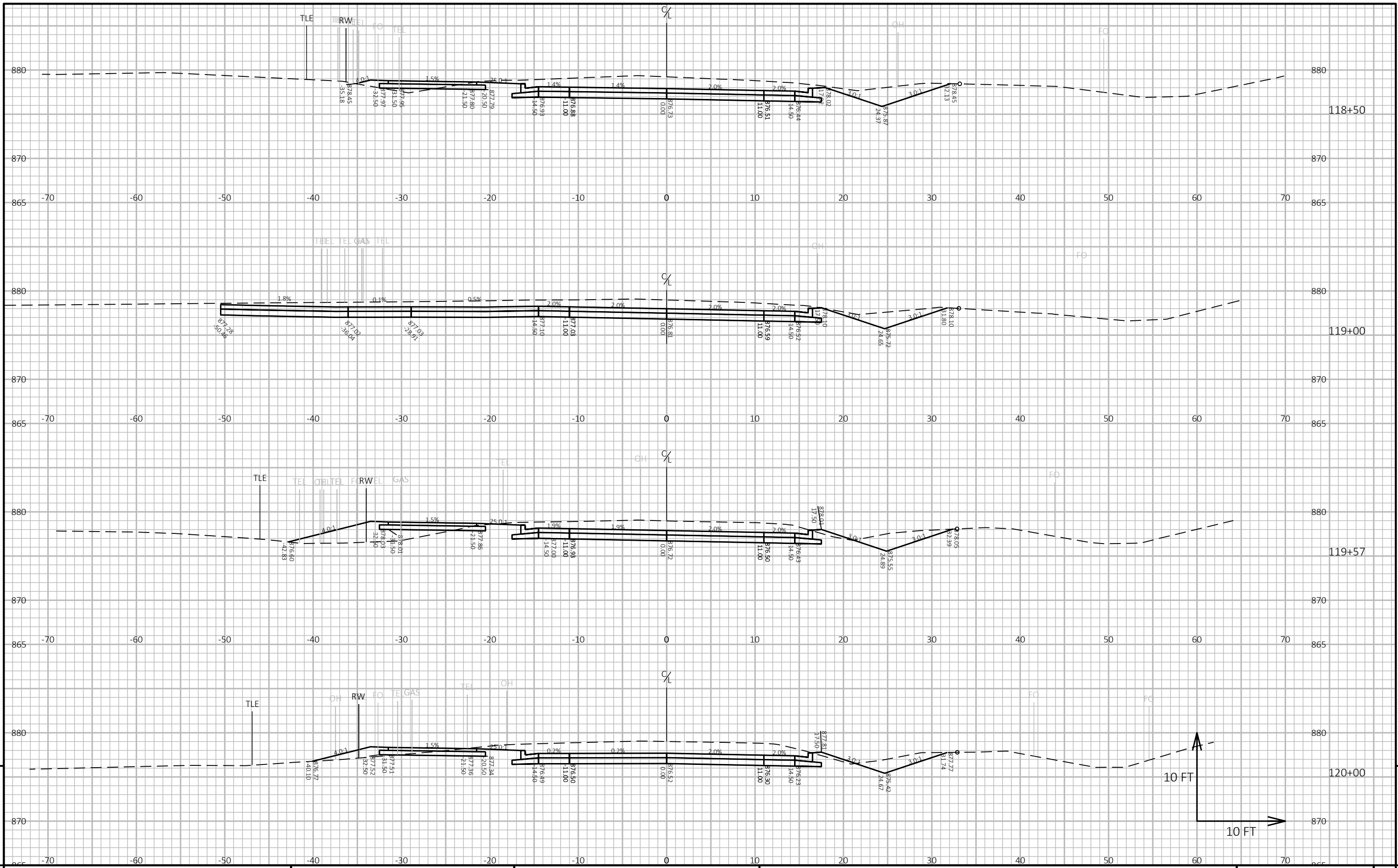
HWY: S HASTINGS WAY

COUNTY: EAU CLAIRE

CROSS SECTIONS: S HASTINGS WAY

SHEET

E

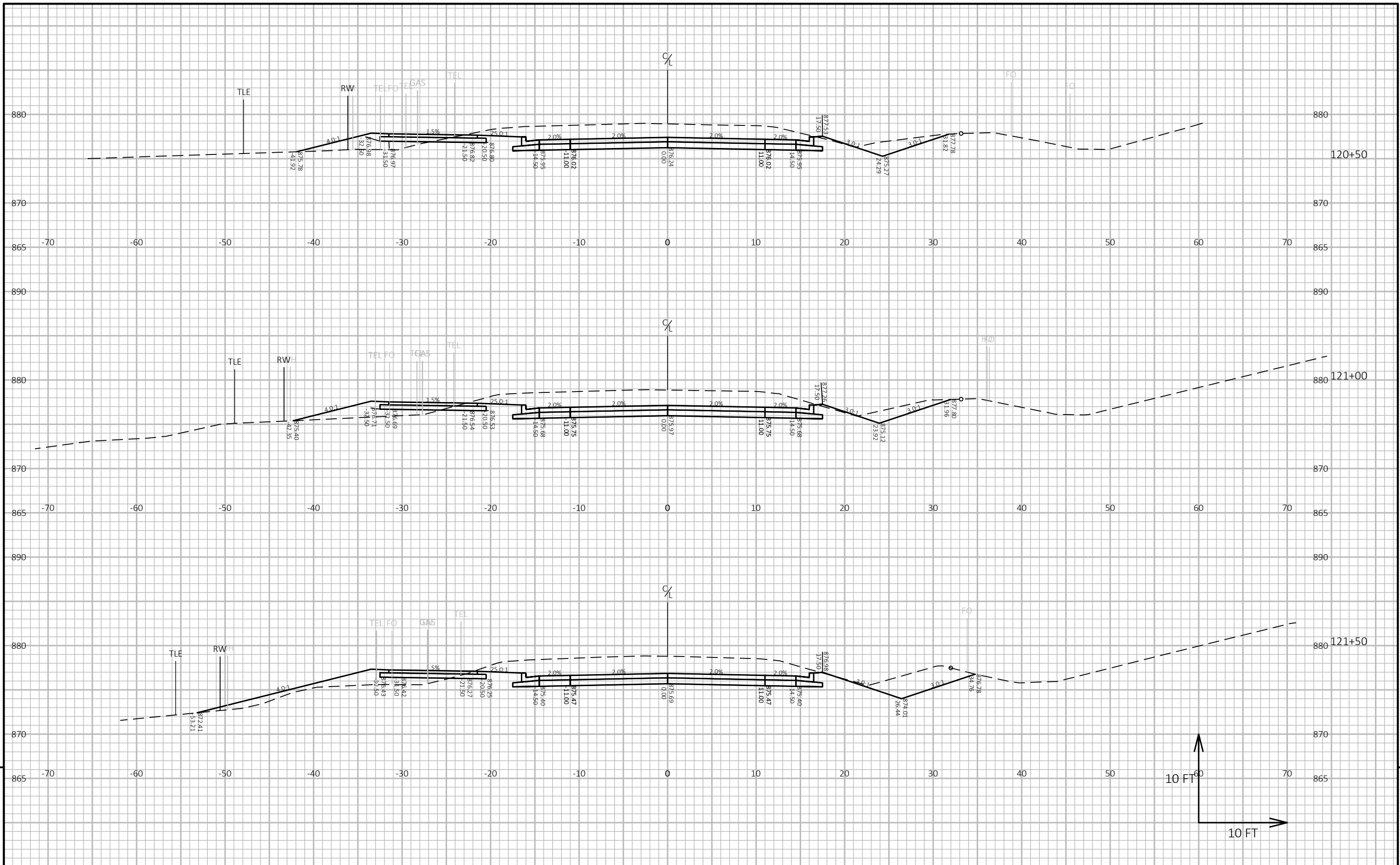


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LAYOUT NAME - 090214-xs



PROJECT NO: 7995-02-65

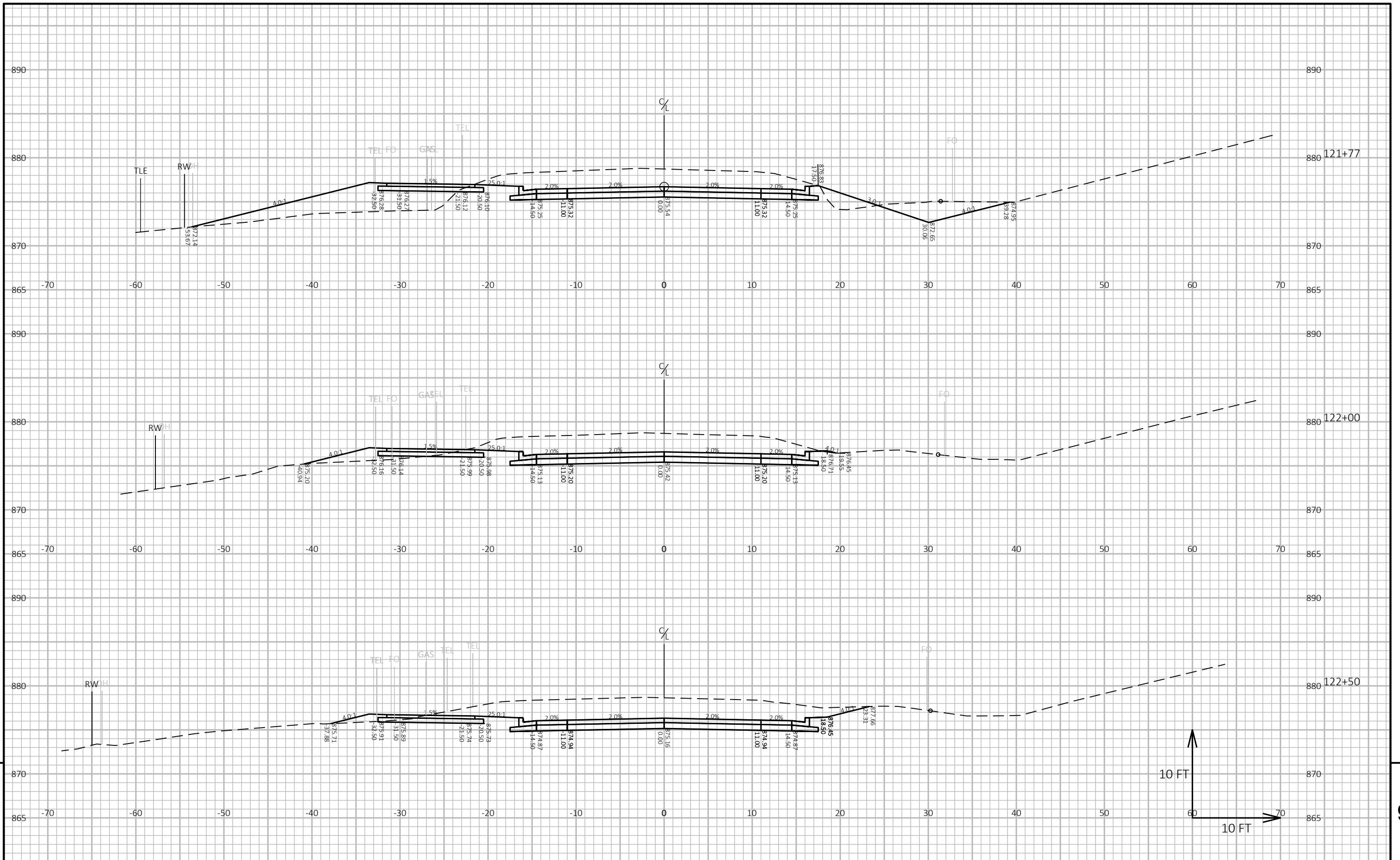
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COUNTY: EAU CLAIRE

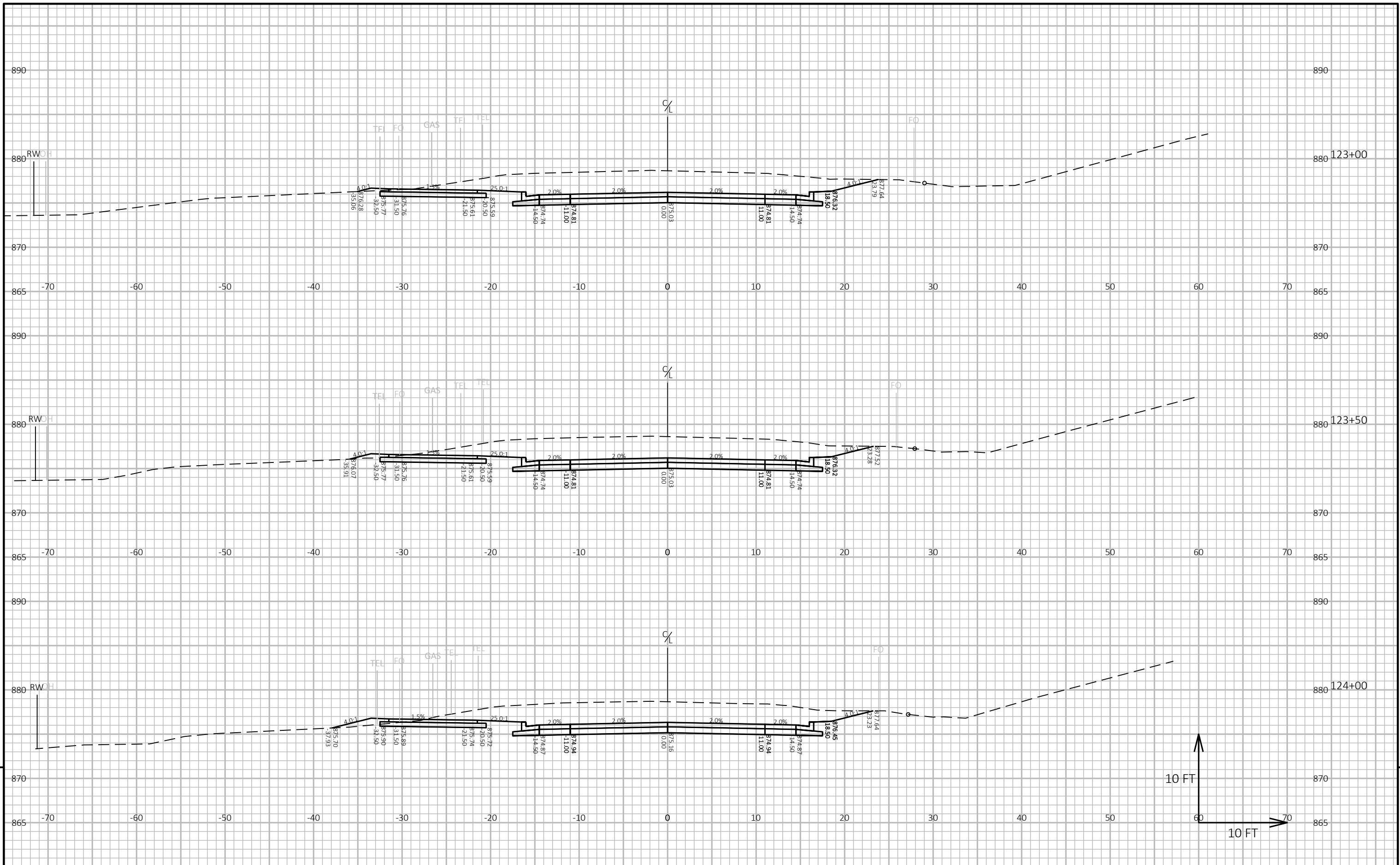
CROSS SECTIONS: S HASTINGS WAY

SHEET

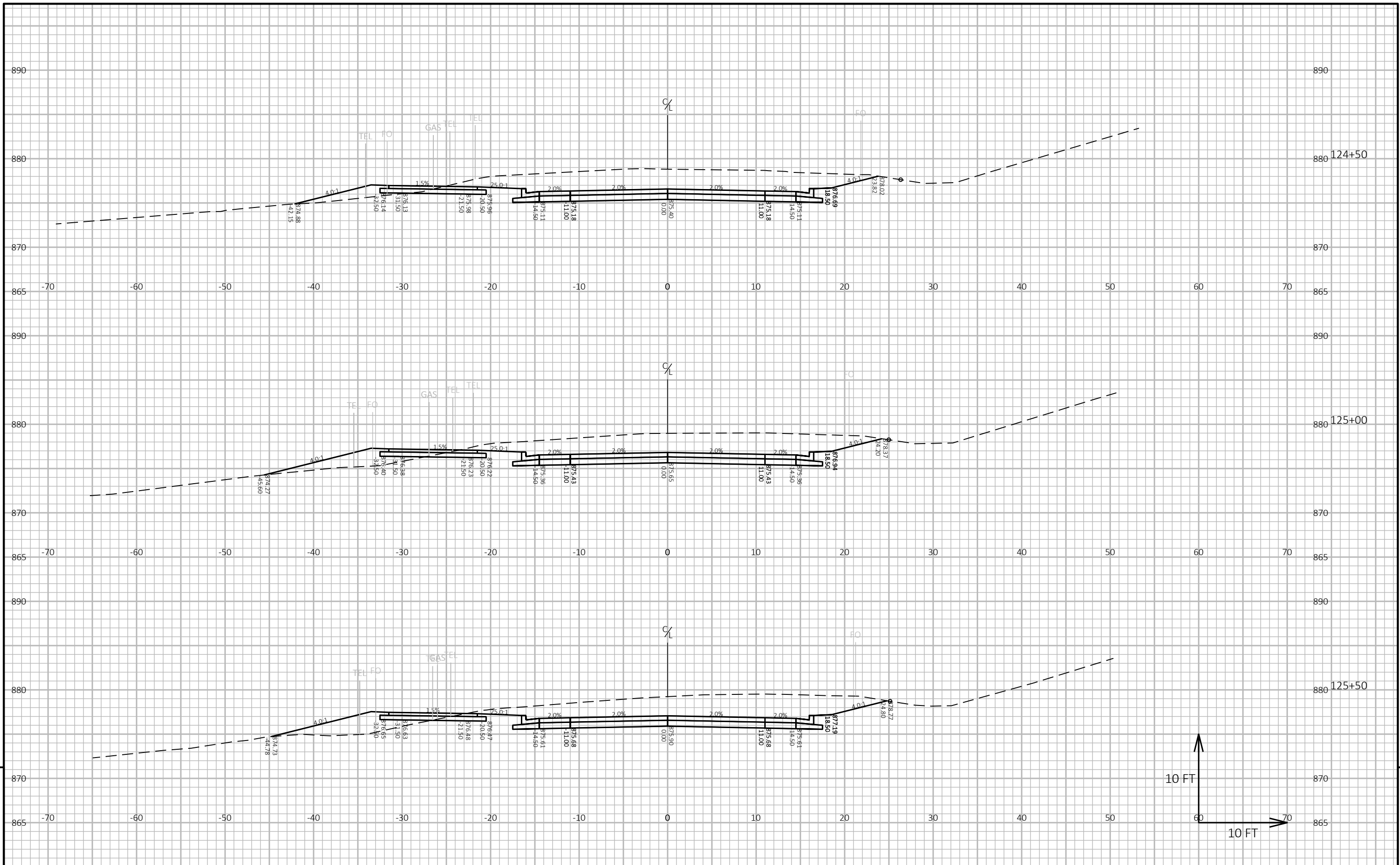
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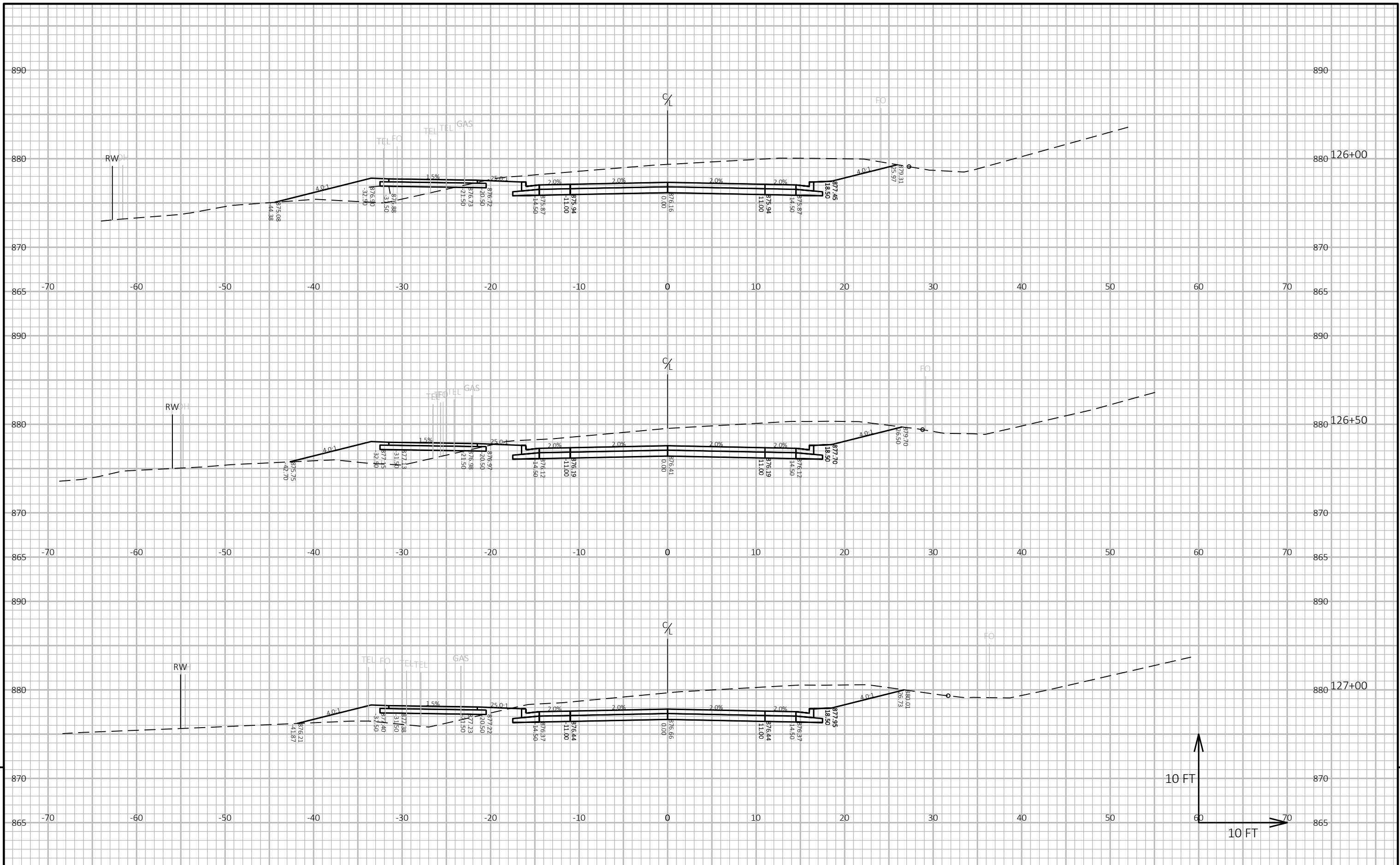


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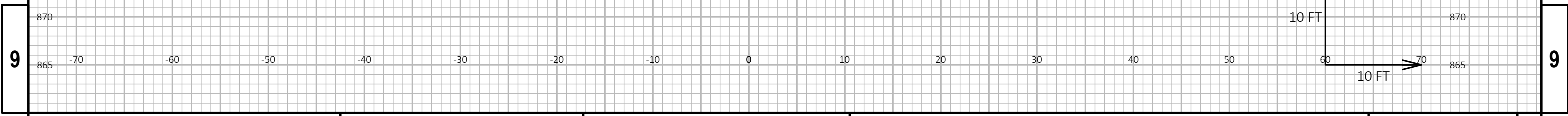


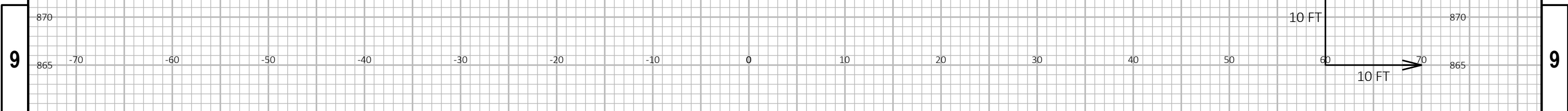
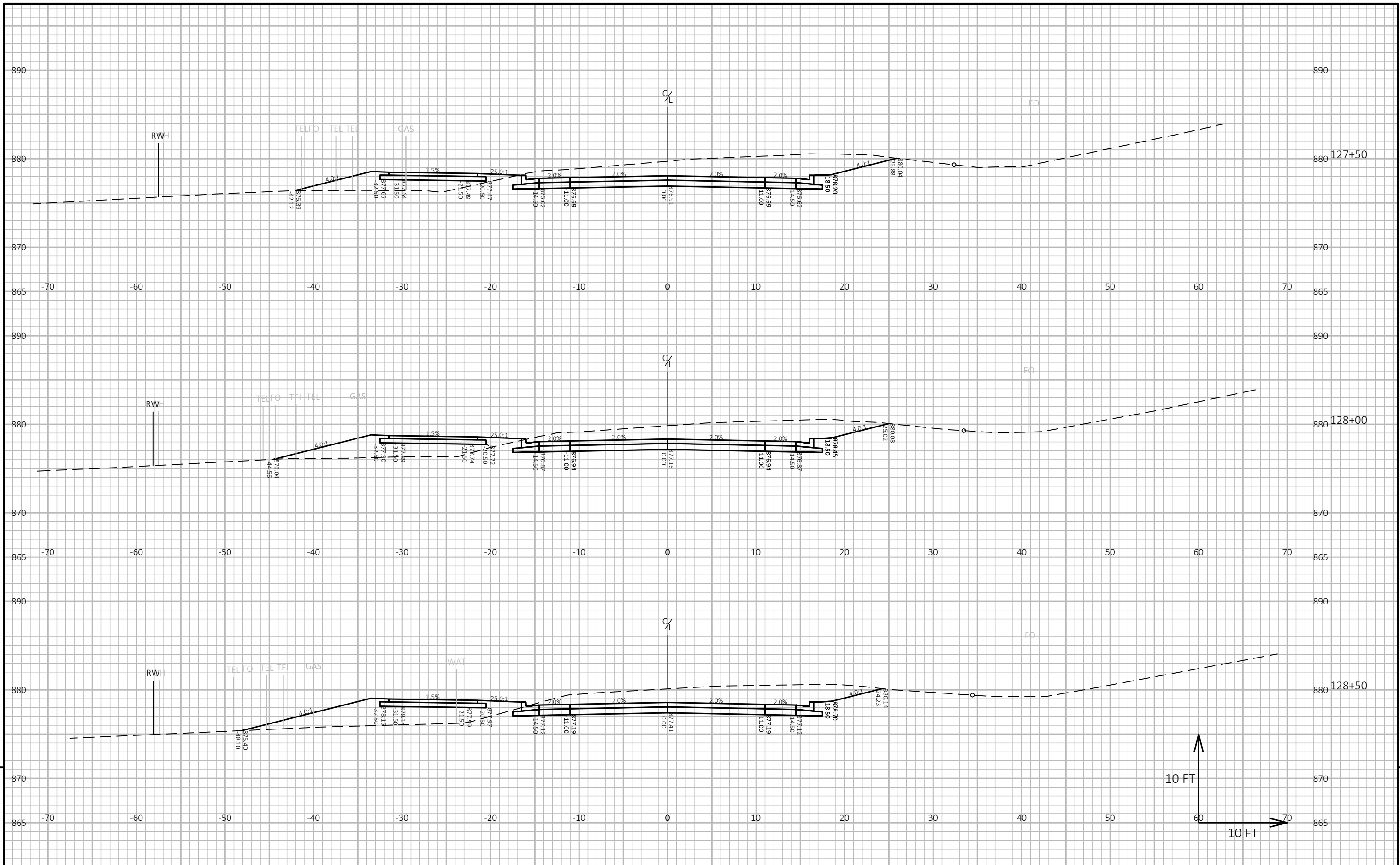
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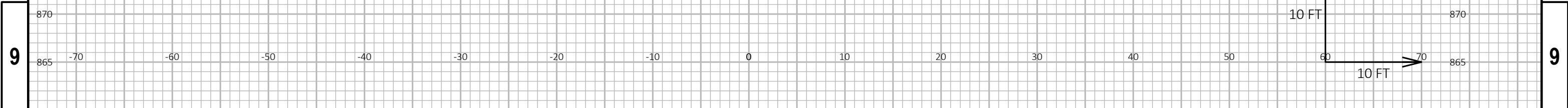
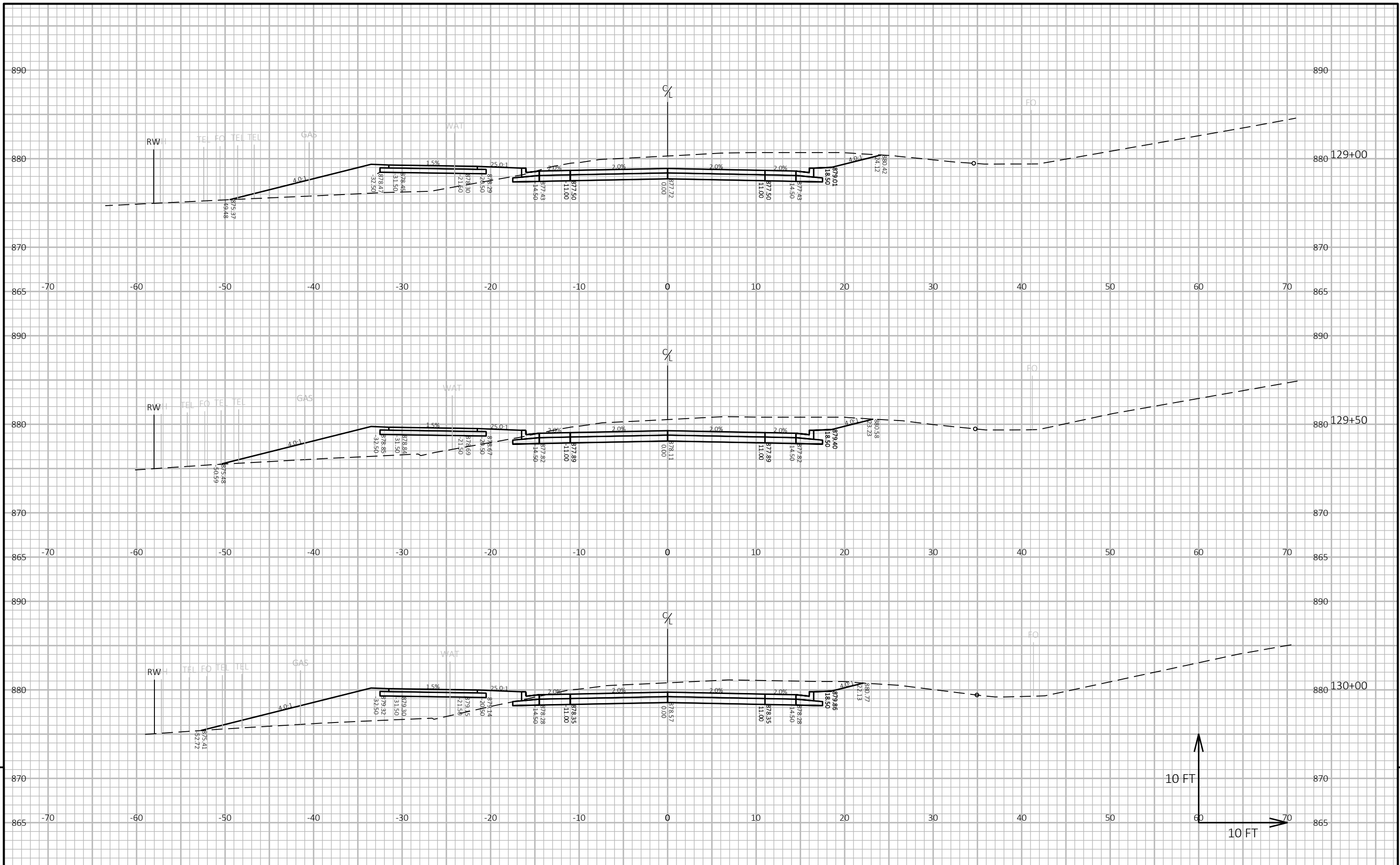


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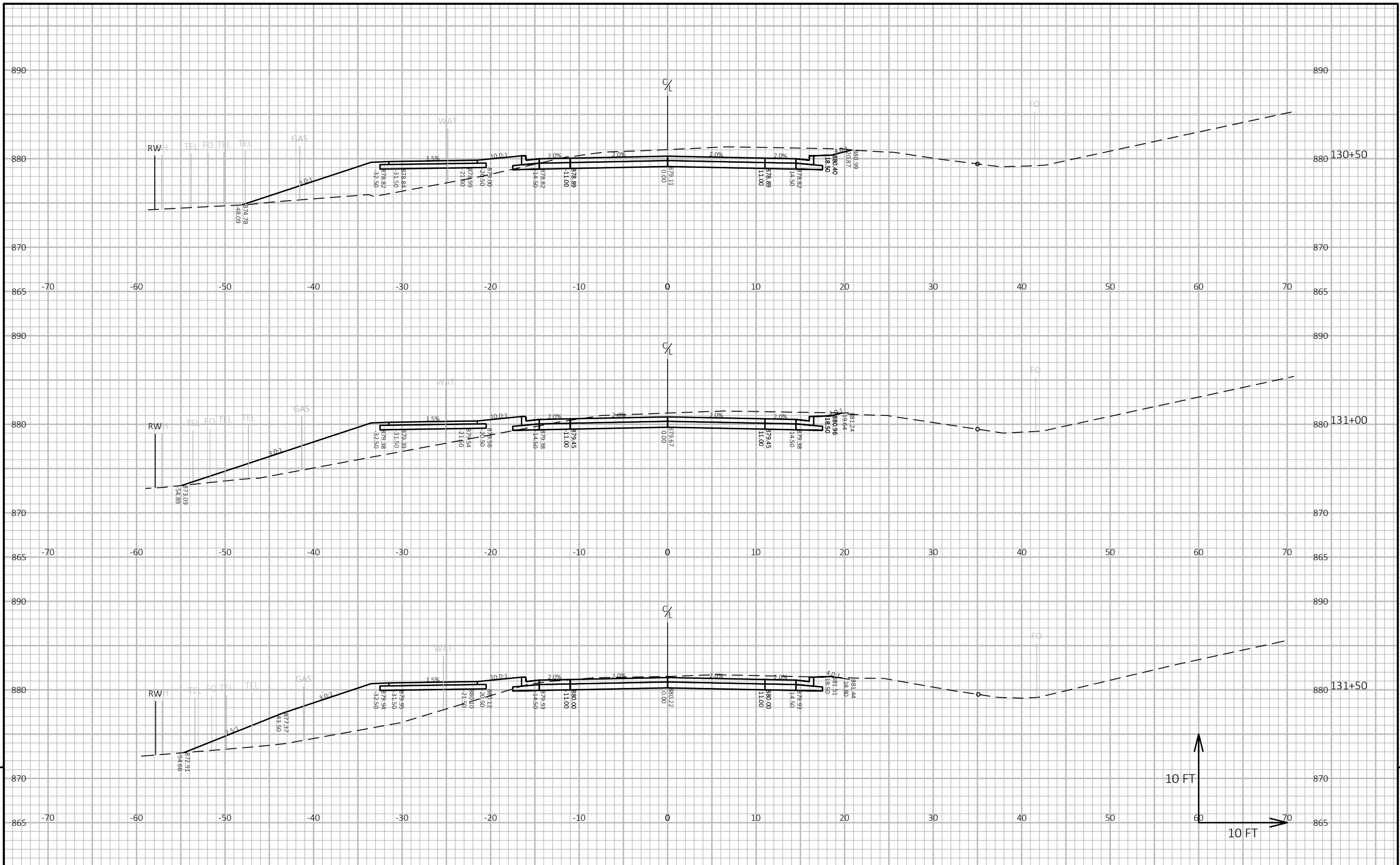




PROJECT NO: 7995-02-65 HWY: S HASTINGS WAY COUNTY: EAU CLAIRE CROSS SECTIONS: S HASTINGS WAY SHEET E



PROJECT NO: 7995-02-65 HWY: S HASTINGS WAY COUNTY: EAU CLAIRE CROSS SECTIONS: S HASTINGS WAY SHEET E



PROJECT NO: 7995-02-65	HWY: S HASTINGS WAY	COUNTY: EAU CLAIRE	CROSS SECTIONS: S HASTINGS WAY	SHEET	E
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Notes



Wisconsin Department of Transportation

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