

ORDER OF SHEETS

Section No.	1	Title
Section No.	2	Typical Sections and Details
Section No.	3	Estimate of Quantities
Section No.	3	Miscellaneous Quantities
Section No.	4	Right of Way Plat
Section No.	5	Plan and Profile
Section No.	6	Standard Detail Drawings
Section No.	7	Sign Plates
Section No.	8	Structure Plans
Section No.	9	Computer Earthwork Data
Section No.	9	Cross Sections

TOTAL SHEETS = 210

PROJECT ID: 4075-35-71/72

23

COUNTY: BROWN & OUTAGAMIE

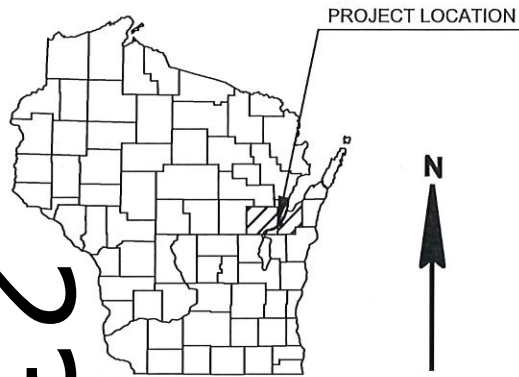
STATE OF WISCONSIN
 DEPARTMENT OF TRANSPORTATION
 PLAN OF PROPOSED IMPROVEMENT

KAUKAUNA - WRIGHTSTOWN
 CLARIBEL STREET - CTH JJ CTH JJ - CTH D
 STH 96 STH 96
 OUTAGAMIE COUNTY BROWN & OUTAGAMIE COUNTY

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
4075-35-71	WISC 2024014	1
4075-35-72	WISC 2024028	1

STATE PROJECT NUMBER
4075-35-71

STATE PROJECT NUMBER
4075-35-72



DESIGN DESIGNATION

A.A.D.T. (2024)	=	5070
A.A.D.T. (2044)	=	5580
D.H.V.	=	610
D.D.	=	60/40
T.	=	10.9%
DESIGN SPEED	=	25-55 MPH
ESALS	=	1,500,000

CONVENTIONAL SYMBOLS

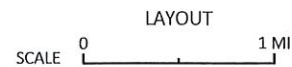
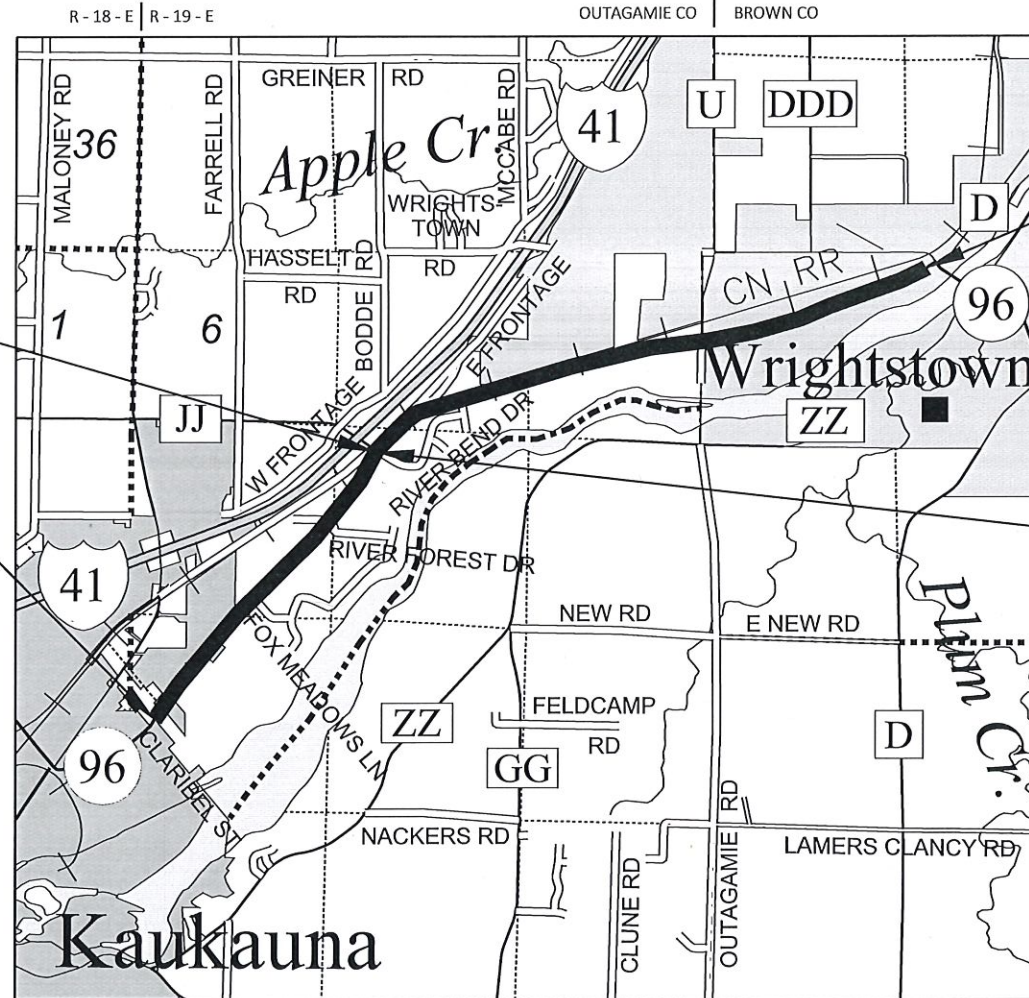
PLAN	PROFILE
CORPORATE LIMITS	GRADE LINE
PROPERTY LINE	ORIGINAL GROUND
LOT LINE	MARSH OR ROCK PROFILE (To be noted as such)
LIMITED HIGHWAY EASEMENT	SPECIAL DITCH
EXISTING RIGHT OF WAY	GRADE ELEVATION
PROPOSED OR NEW R/W LINE	CULVERT (Profile View)
SLOPE INTERCEPT	UTILITIES
REFERENCE LINE	ELECTRIC
EXISTING CULVERT	FIBER OPTIC
PROPOSED CULVERT (Box or Pipe)	GAS
COMBUSTIBLE FLUIDS	SANITARY SEWER
	STORM SEWER
MARSH AREA	TELEPHONE
	WATER
	UTILITY PEDESTAL
WOODED OR SHRUB AREA	POWER POLE
	TELEPHONE POLE

END PROJECT 4075-35-71
 STA 129+04.00
 Y = 581036.674
 X = 874489.676

BEGIN PROJECT 4075-35-71
 STA 33+50.00
 Y = 573659.995
 X = 868488.460

END PROJECT 4075-35-72
 STA 291+71.36
 Y = 586165.174
 X = 889644.931

BEGIN PROJECT 4075-35-72
 STA 129+04.00
 Y = 581036.674
 X = 874489.676

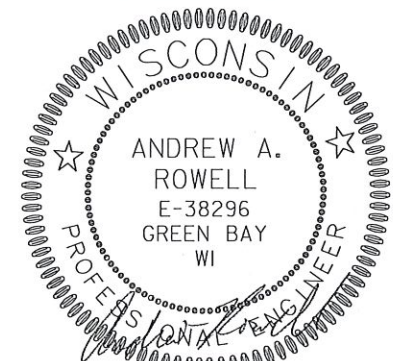


TOTAL NET LENGTH OF CENTERLINE = 4.890 MI
 4075-35-71 LENGTH OF CENTERLINE = 1.809 MI
 4075-35-72 LENGTH OF CENTERLINE = 3.081 MI

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS), OUTAGAMIE COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES. ELEVATIONS ARE REFERENCED TO NAVD 88 (2012). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 12A.

ORIGINAL PLANS PREPARED BY

AVRES



ANDREW A. ROWELL
 E-38296
 GREEN BAY
 WI

1/13/2023
 (Date)

STATE OF WISCONSIN
 DEPARTMENT OF TRANSPORTATION

PREPARED BY
 Surveyor _____ ROBERT E LEE/WISDOT NE REGION
 Designer _____ AYRES
 Project Manager _____ JESSE HANSEN
 Regional Supervisor _____ TAMMY RABE

APPROVED FOR THE DEPARTMENT
 DATE: 1/13/2023 _____ (Signature)

E

GENERAL NOTES

THE LOCATIONS OF EXISTING UTILITY FACILITIES AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY FACILITIES WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

NO TREES OR SHRUBS ARE TO BE REMOVED WITHOUT APPROVAL OF THE ENGINEER.

INLET AND DISCHARGE ELEVATIONS FOR CULVERTS SHOWN ON THE PLAN MAY BE ADJUSTED BY THE ENGINEER TO FIT FIELD CONDITIONS.

PROPERTY LINES SHOWN ARE APPROXIMATE.

TEMPORARY STORAGE OF ANY EQUIPMENT OR EXCAVATED MATERIAL WILL NOT BE PERMITTED IN WETLANDS, FLOODWAY OR FLOODPLAIN OF ANY WETLANDS.

PLACE EROSION CONTROL DEVICES IN SEQUENCE WITH CONSTRUCTION OPERATIONS OR AS DETERMINED BY THE ENGINEER. EROSION CONTROL FEATURES ARE SHOWN IN APPROXIMATE.

TRAFFIC CONTROL DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

SOME LOCATIONS OF PIPE SPECIFIED IN THE PLAN REQUIRE PIPE LENGTHS OTHER THAN 8-FOOT SECTIONS.

REFERENCE LINE REFERS TO ALIGNMENT AND STATIONING. CENTERLINE REFERS TO EXISTING ROADWAY CENTERLINE. REFERENCE LINE AND CENTERLINE ARE NOT COINCIDENTAL.

SAW CUT LOCATIONS SHOWN ON THE PLAN ARE SUBJECT TO ADJUSTMENT BY THE ENGINEER IN THE FIELD. THE LINE OF SUCH SAW CUTS WILL BE NEATLY DELINEATED THRU THE ASPHALT OR CONCRETE WITHOUT ANY DAMAGE TO THE REMAINING PORTION OF THE EXISTING PAVEMENT.

CURVE DATA IS BASED ON ARC DEFINITION.

THE EXACT LOCATION AND WIDTH OF PRIVATE ENTRANCES WILL BE DETERMINED IN THE FIELD BY THE ENGINEER. ALL DRIVEWAYS ARE TO BE REPLACED IN KIND UNLESS OTHERWISE DIRECTED BY THE ENGINEER OR AS SHOWN ON THE PLANS. BASE AGGREGATE DENSE 1 1/4-INCH WILL BE USED UNDER ALL DRIVEWAYS.

ALL TIES ON THIS PLAN ARE HORIZONTAL UNLESS DESCRIBED OTHERWISE.

RADIUS POINTS, UNLESS OTHERWISE NOTED, ARE TO FLANGE OF CURB.

DISTURBED AREAS WITHIN THE RIGHT-OF-WAY ARE TO BE COVERED WITH TOPSOIL, FERTILIZER, SEED AND EROSION MAT.

UTILITIES CONTACTS

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matthew.schaeve@wisconsin.gov

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DESIGNER CONTACT

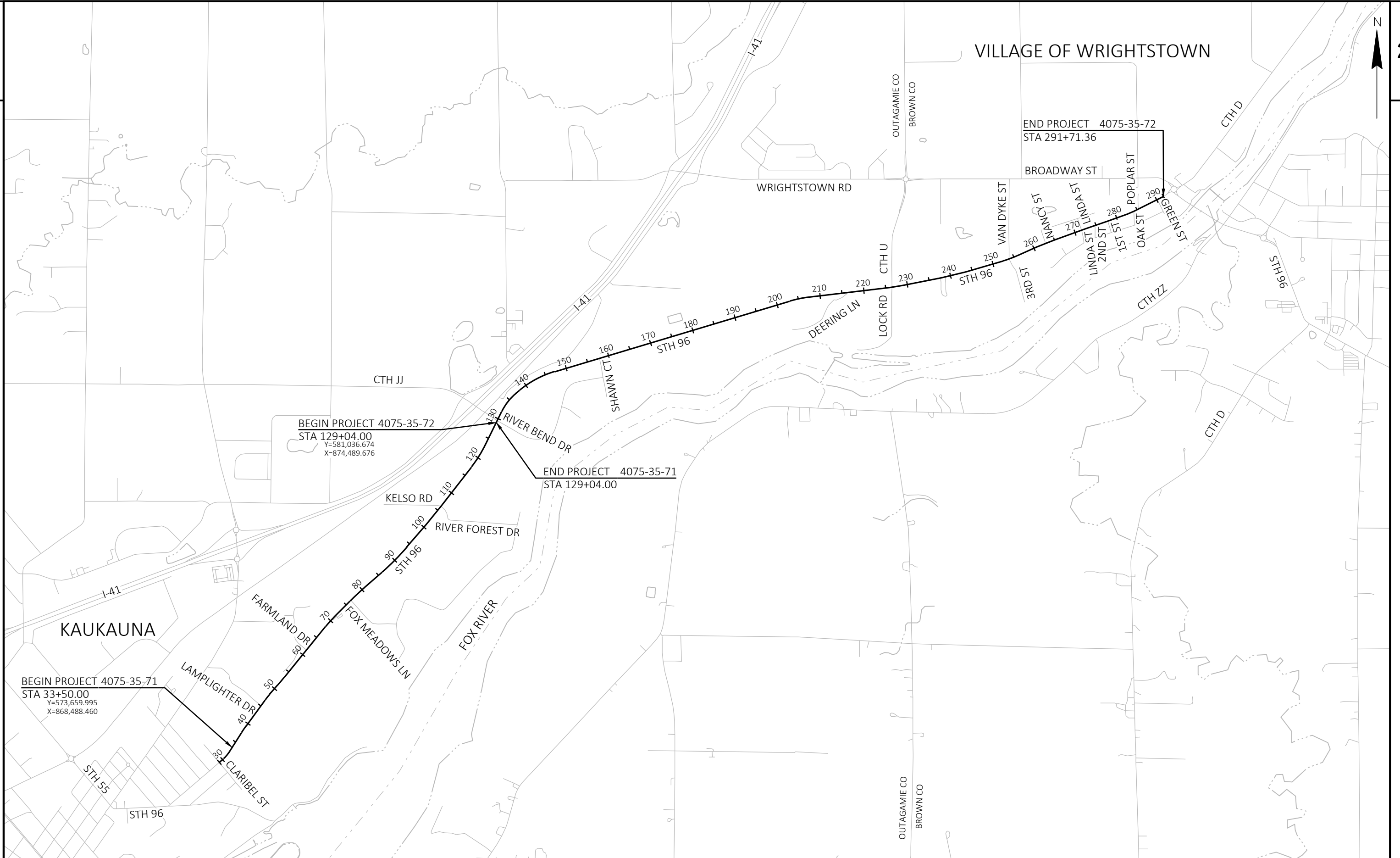
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ASHWAUBENON, WI 54115
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rowella@ayresassociates.com

ORDER OF SECTION 2 DETAIL SHEETS

- GENERAL NOTES
- PROJECT OVERVIEW
- TYPICAL SECTIONS
- CONSTRUCTION DETAILS
- PLAN DETAILS
- CURB RAMP DETAILS
- EROSION CONTROL
- DETOUR SIGNING DETAIL



VILLAGE OF WRIGHTSTOWN



BEGIN PROJECT 4075-35-72
 STA 129+04.00
 Y=581,036.674
 X=874,489.676

END PROJECT 4075-35-72
 STA 291+71.36

END PROJECT 4075-35-71
 STA 129+04.00

BEGIN PROJECT 4075-35-71
 STA 33+50.00
 Y=573,659.995
 X=868,488.460

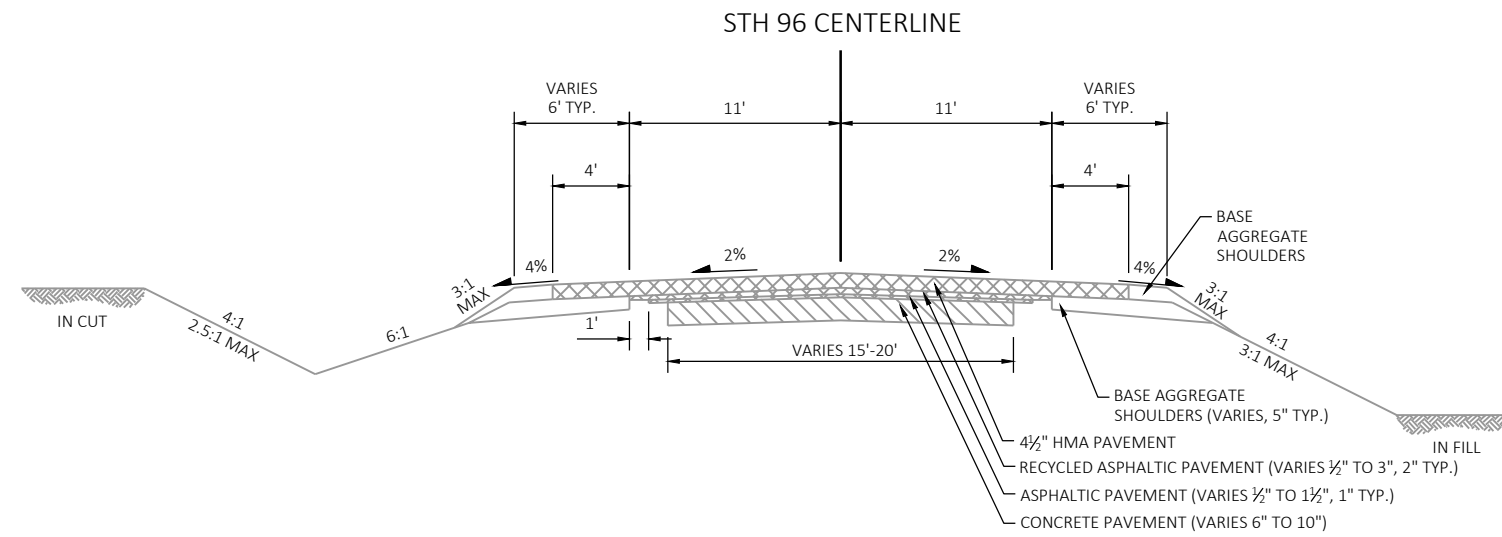
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HWY:	STH 96
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COUNTY:	BROWN & OUTAGAMIE
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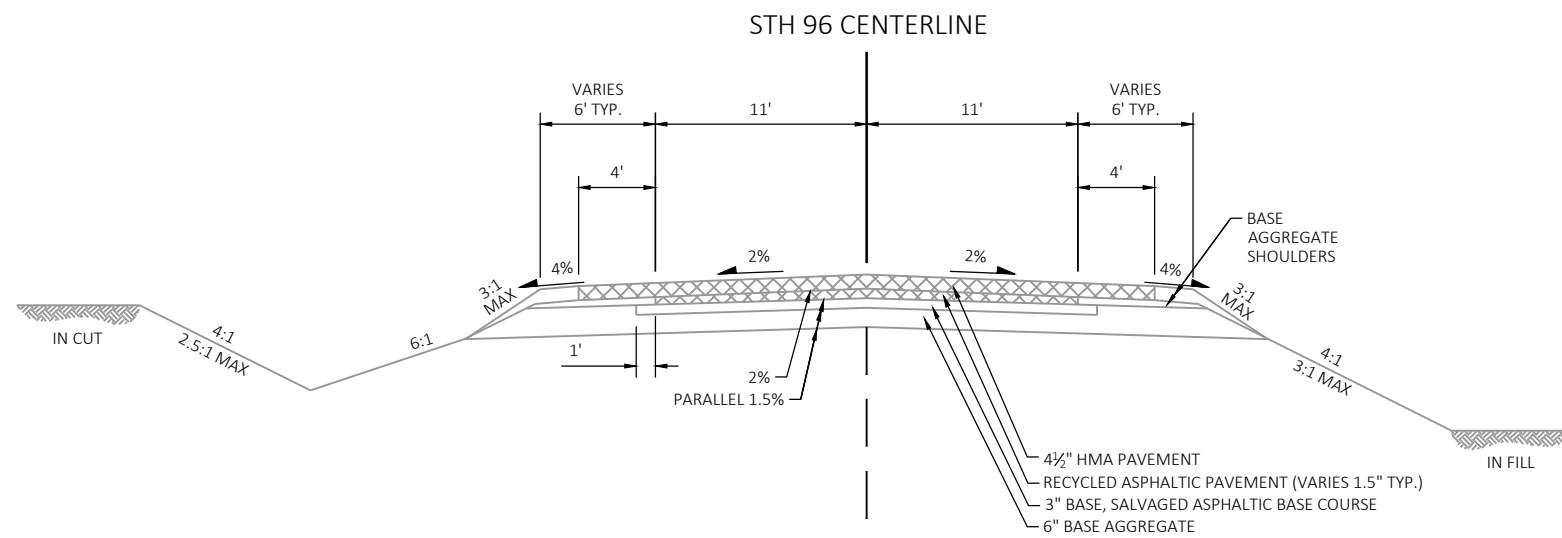
PROJECT OVERVIEW	
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SHEET	E
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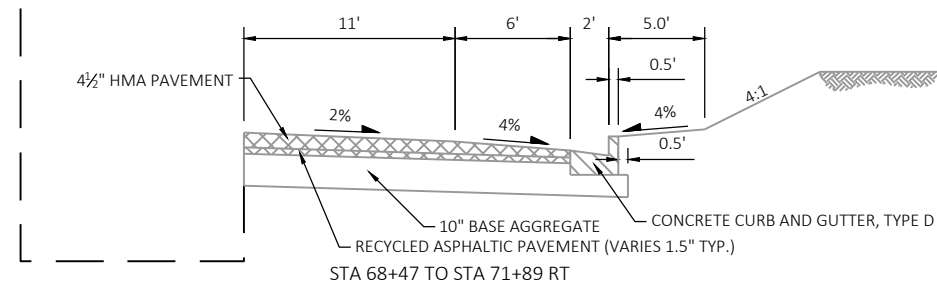
EXISTING TYPICAL SECTION STH 96

STA 33+52 TO STA 61+29
STA 78+29 TO STA 89+29

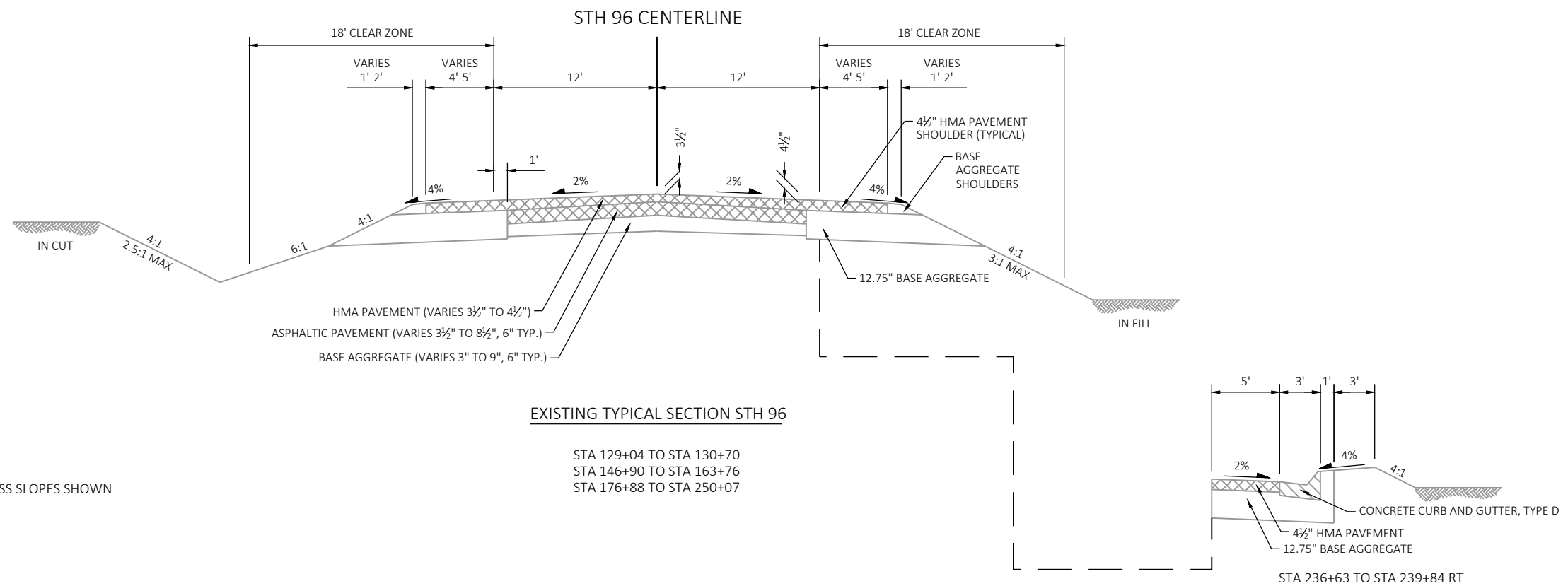
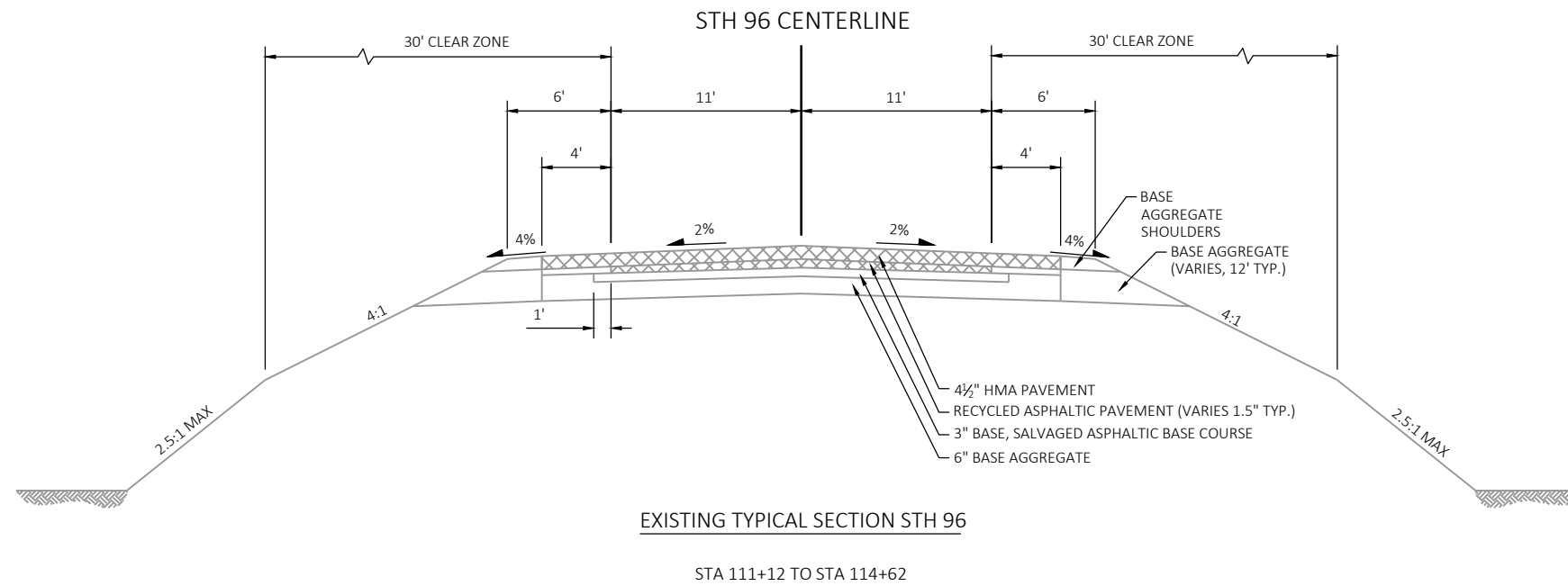


EXISTING TYPICAL SECTION STH 96

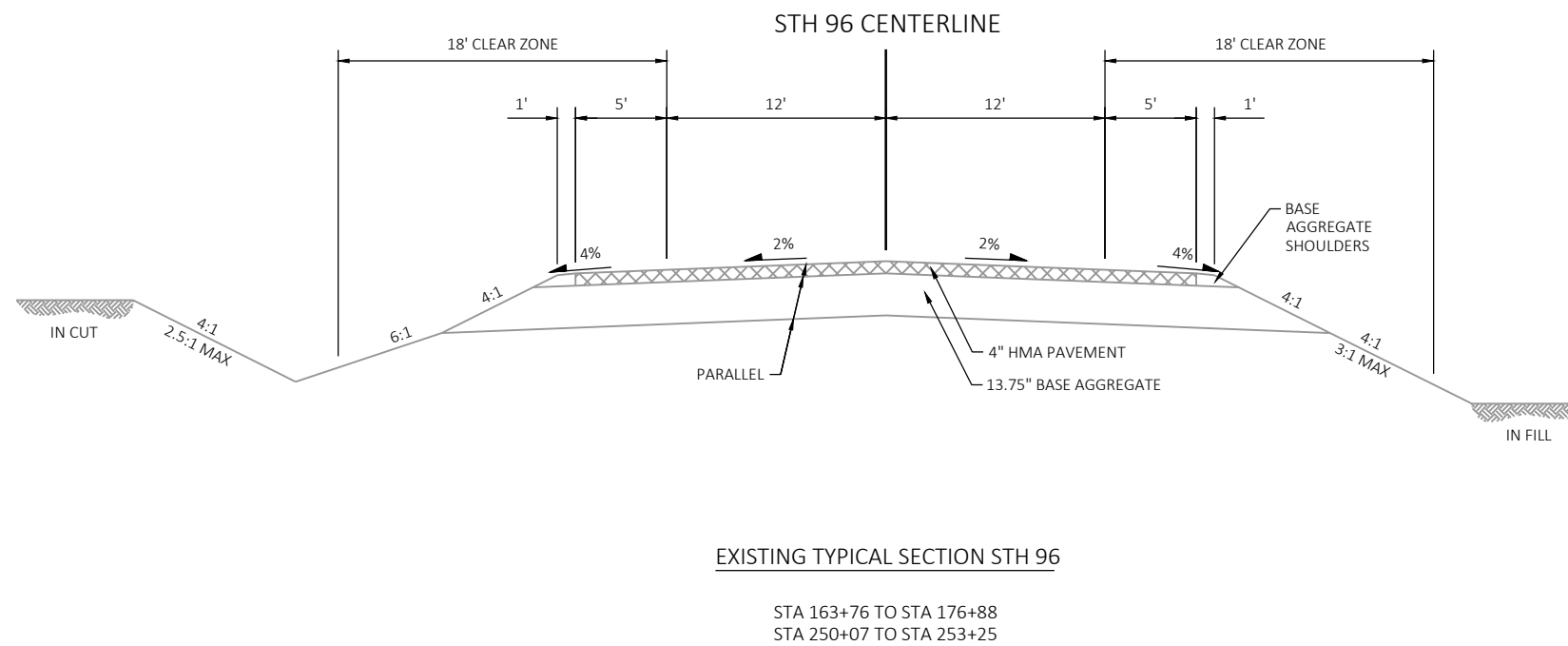
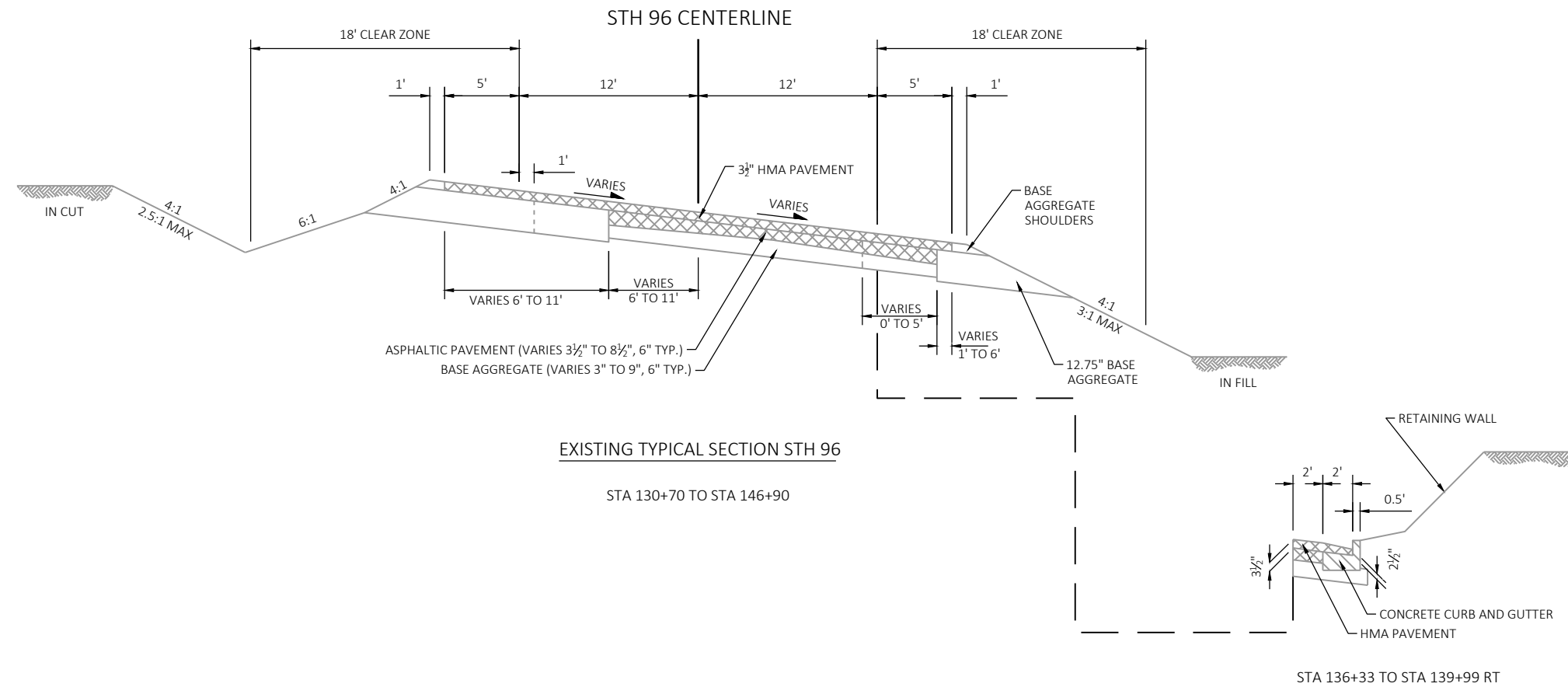
STA 61+29 TO STA 78+29
STA 89+29 TO STA 111+12
STA 114+62 TO STA 129+04



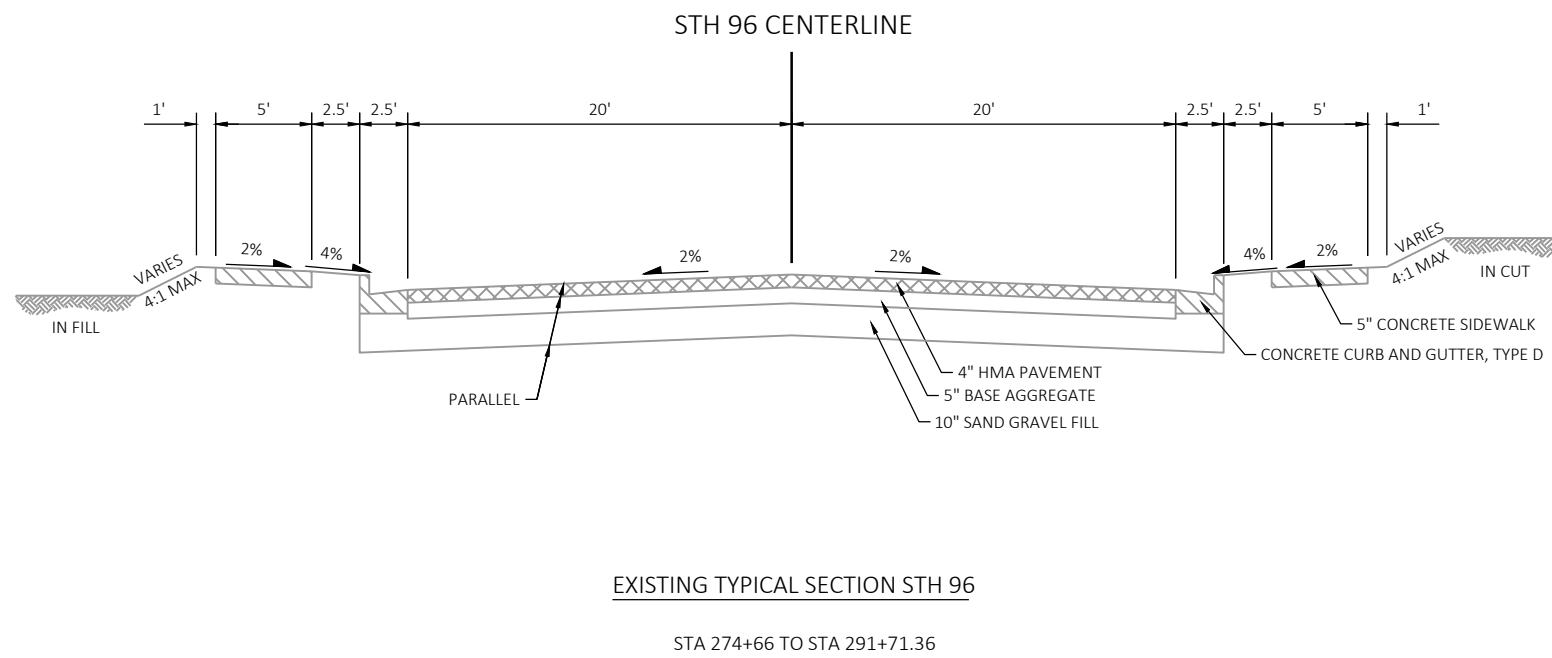
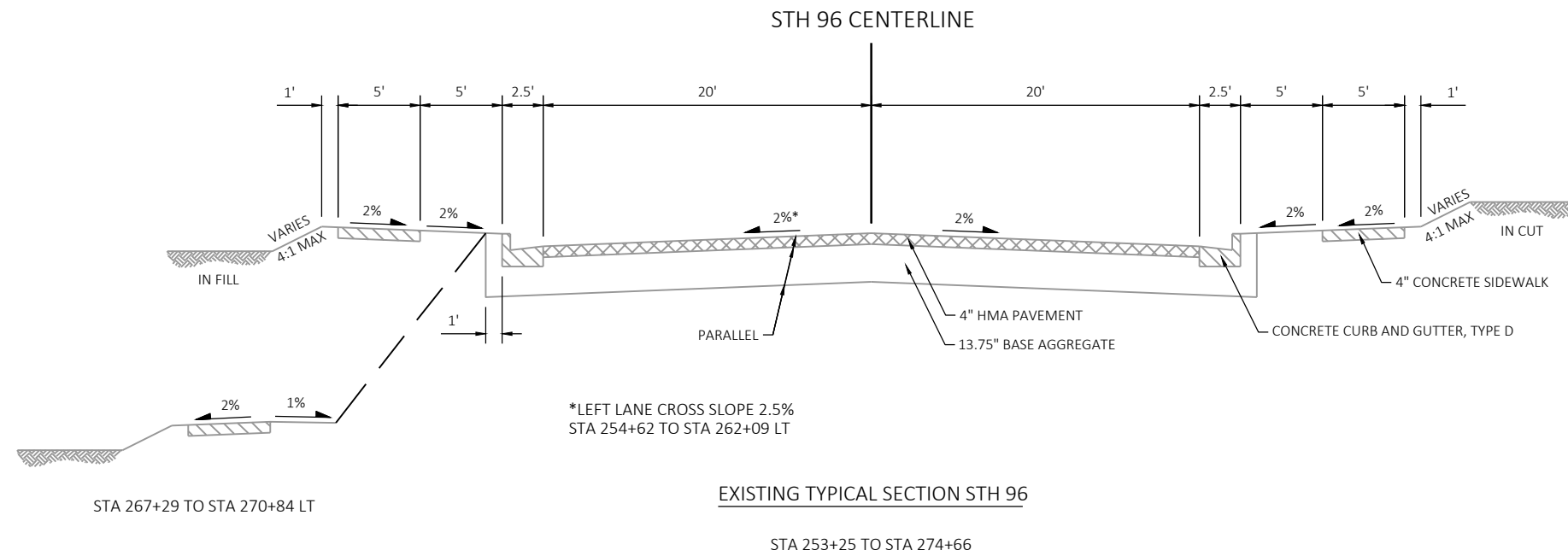
NOTES
CROSS SLOPE VARIES THROUGHOUT; AVERAGE CROSS SLOPES SHOWN



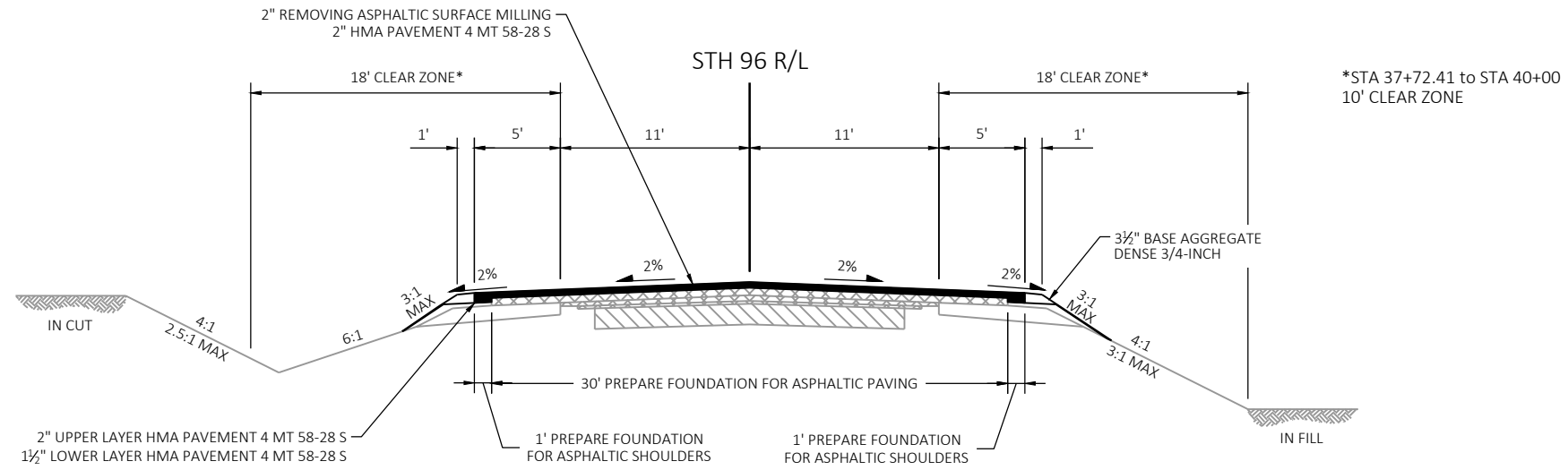
NOTES
CROSS SLOPE VARIES THROUGHOUT; AVERAGE CROSS SLOPES SHOWN



NOTES
CROSS SLOPE VARIES THROUGHOUT; AVERAGE CROSS SLOPES SHOWN

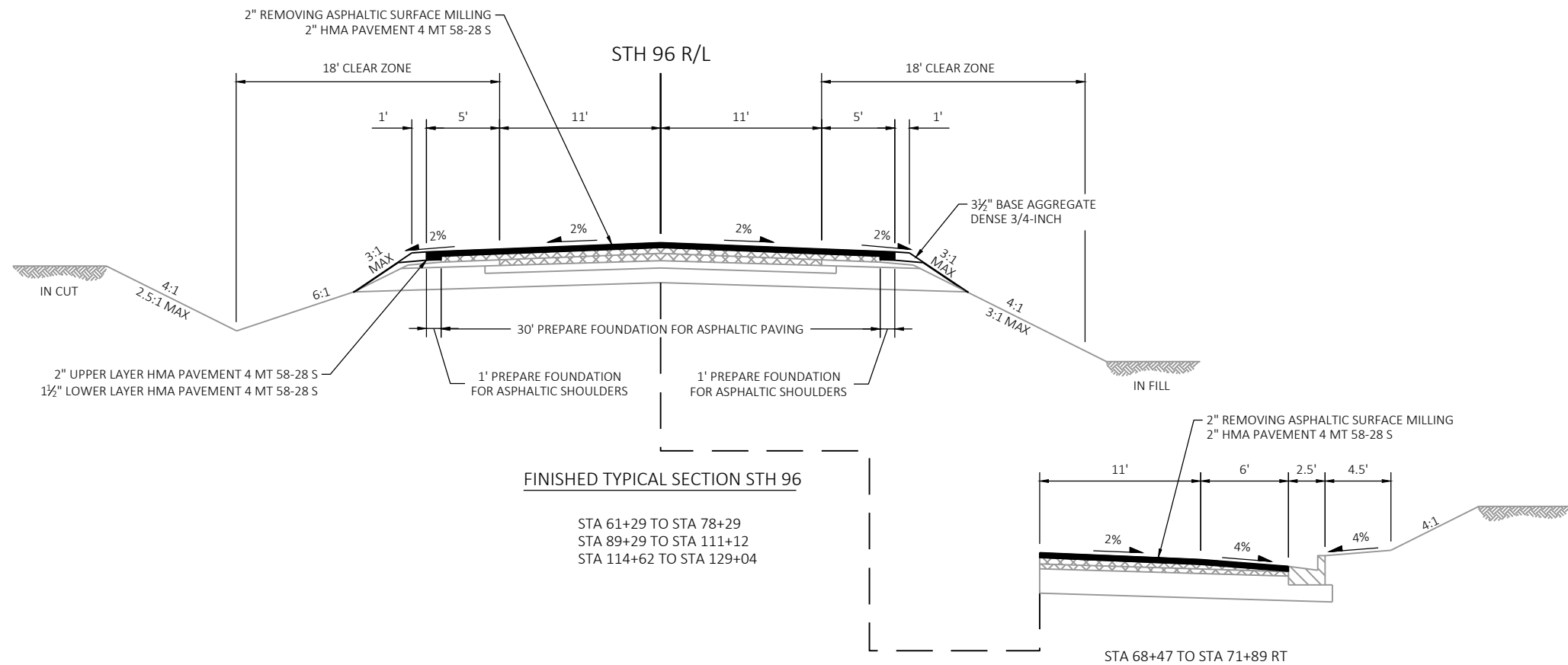


NOTES
CROSS SLOPE VARIES THROUGHOUT; AVERAGE CROSS SLOPES SHOWN



FINISHED TYPICAL SECTION STH 96

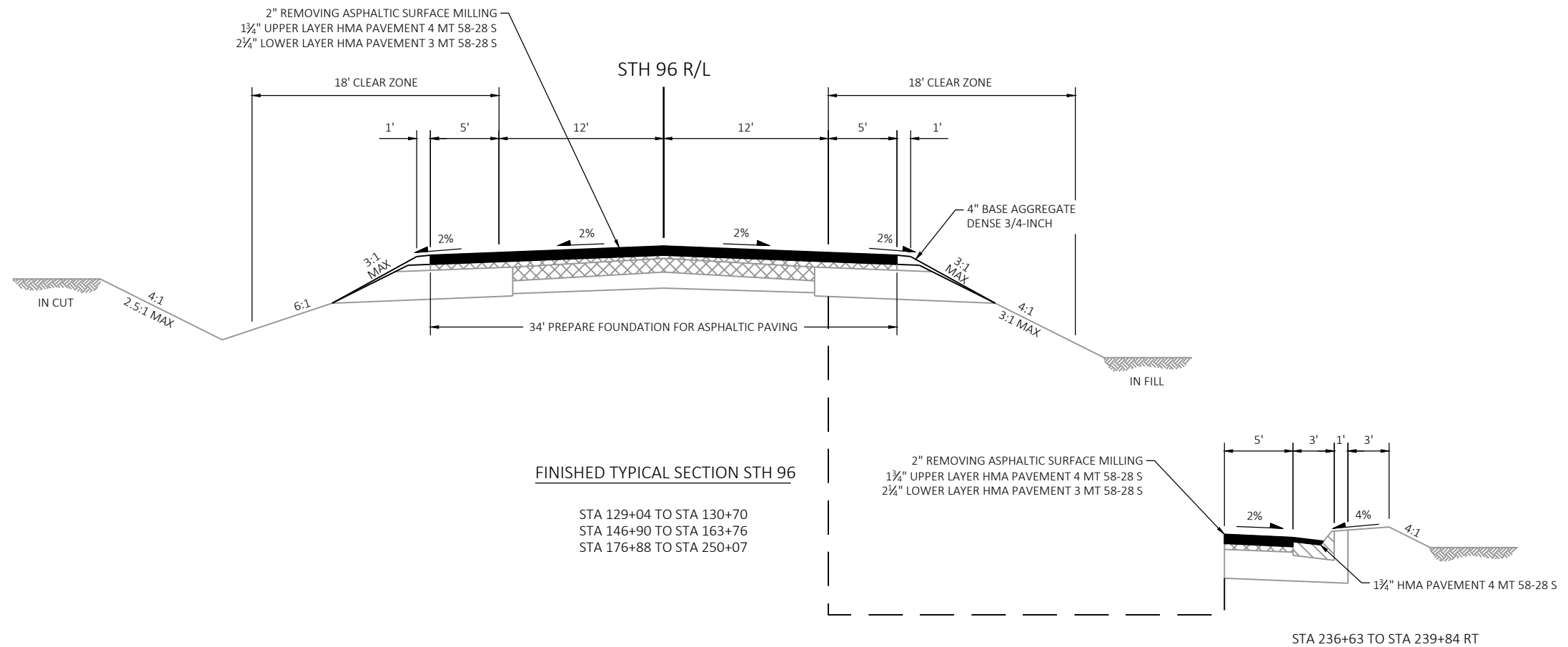
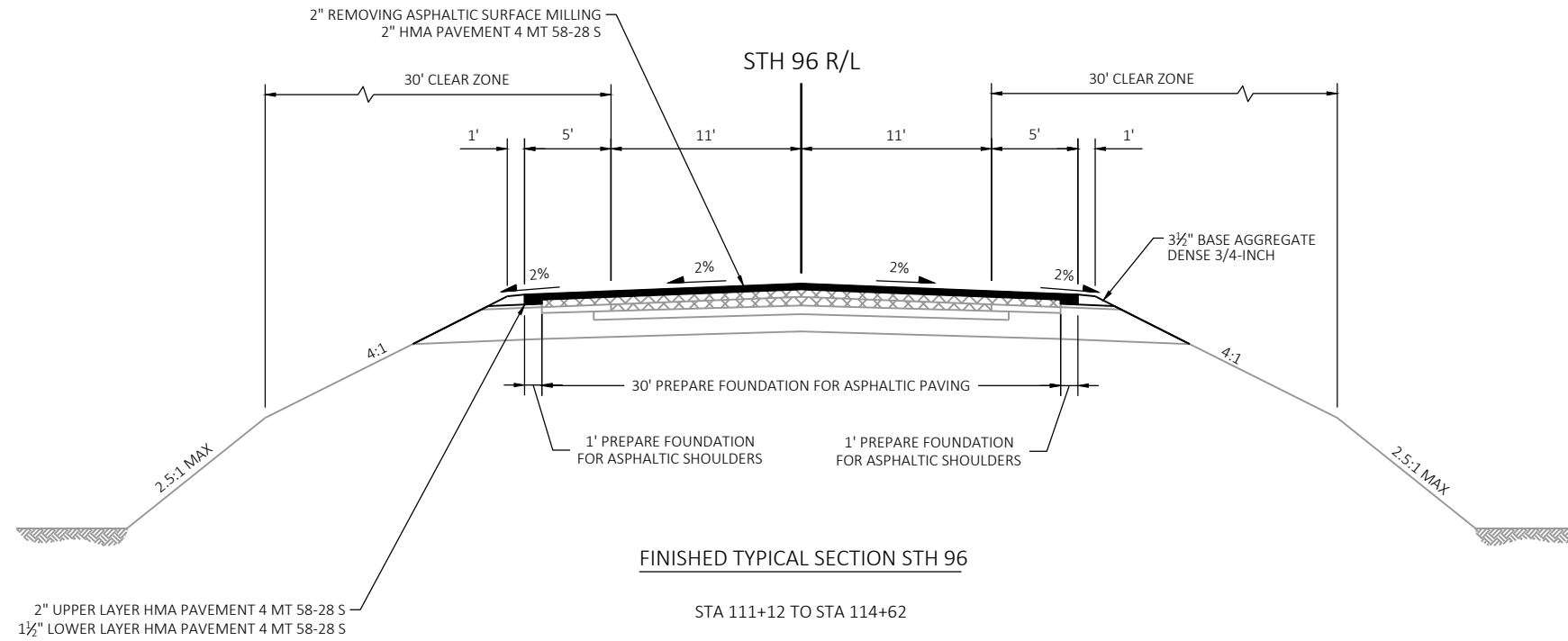
STA 33+52 TO STA 61+29
STA 78+29 TO STA 89+29

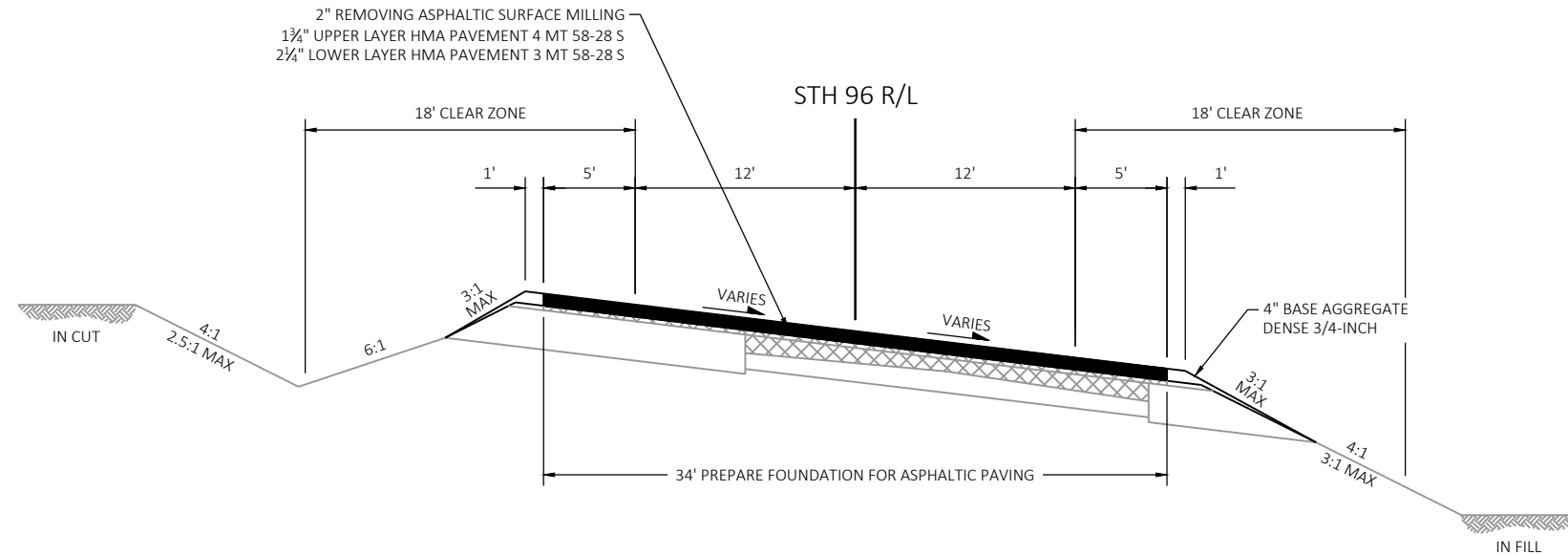


FINISHED TYPICAL SECTION STH 96

STA 61+29 TO STA 78+29
STA 89+29 TO STA 111+12
STA 114+62 TO STA 129+04

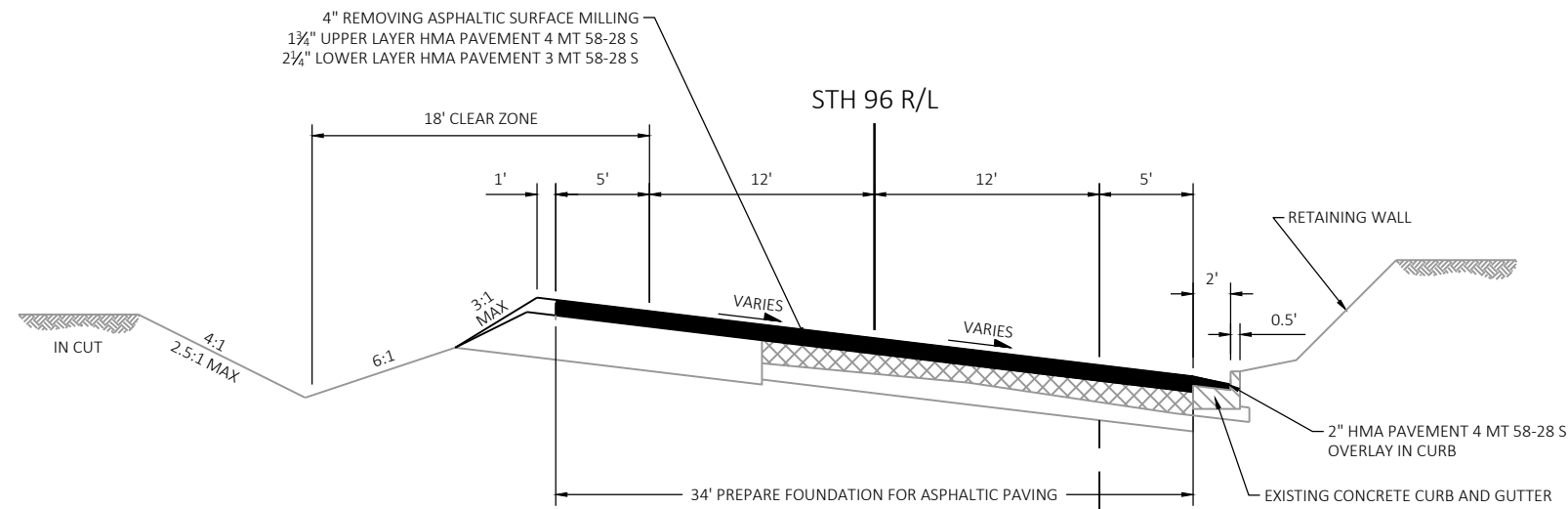
STA 68+47 TO STA 71+89 RT





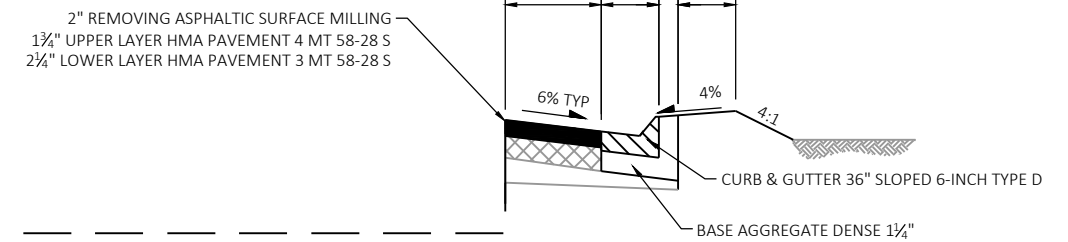
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STA 130+70 TO STA 136+33
STA 139+99 TO STA 146+90

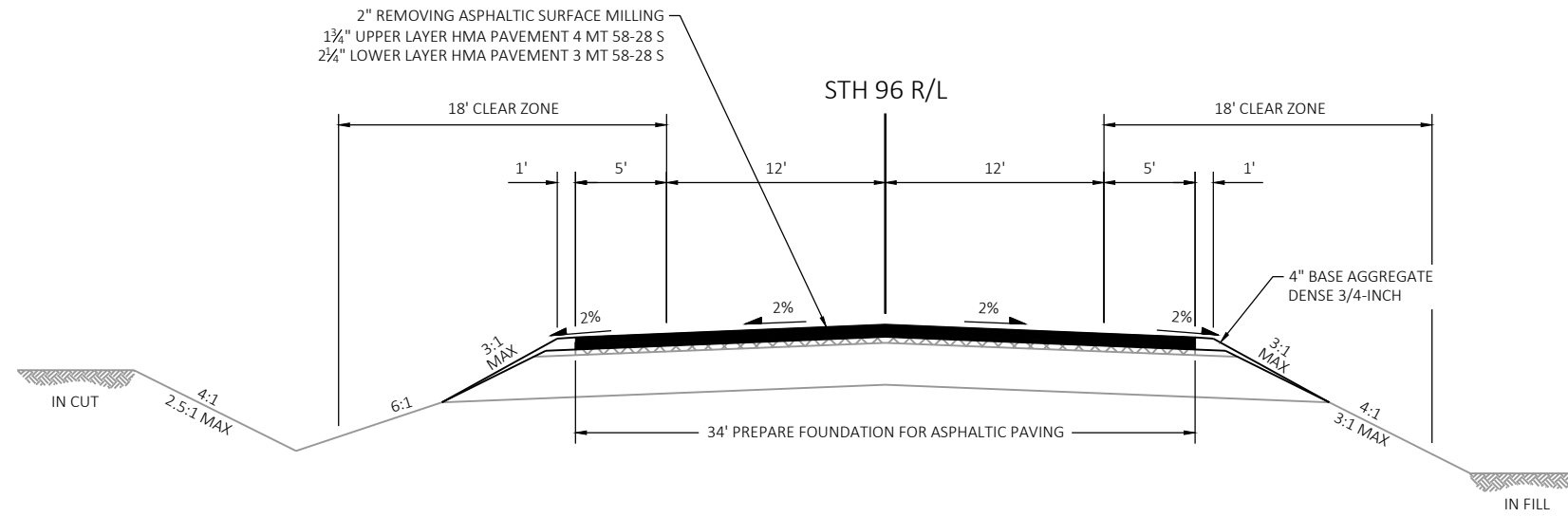


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STA 136+33 TO STA 139+99

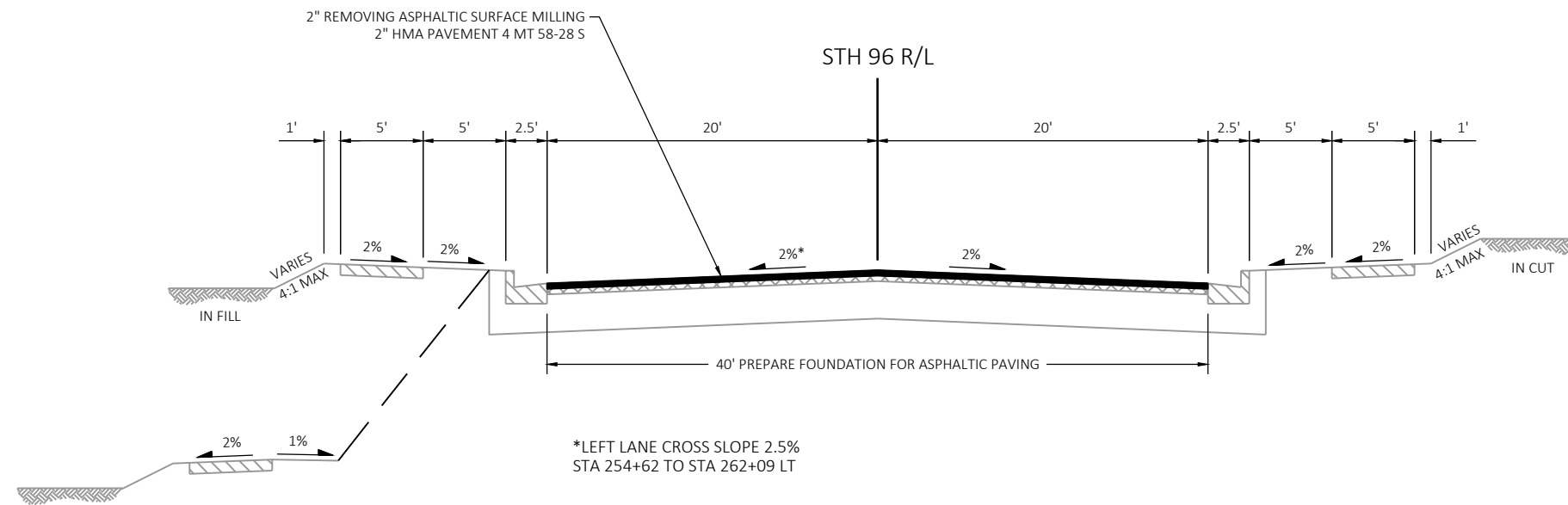


STA 135+60 TO STA 136+45 RT
 ** TRANSITION BETWEEN NEW CURB AND OVERLAY CURB REQUIRED. SEE CONSTRUCTION DETAILS



FINISHED TYPICAL SECTION STH 96

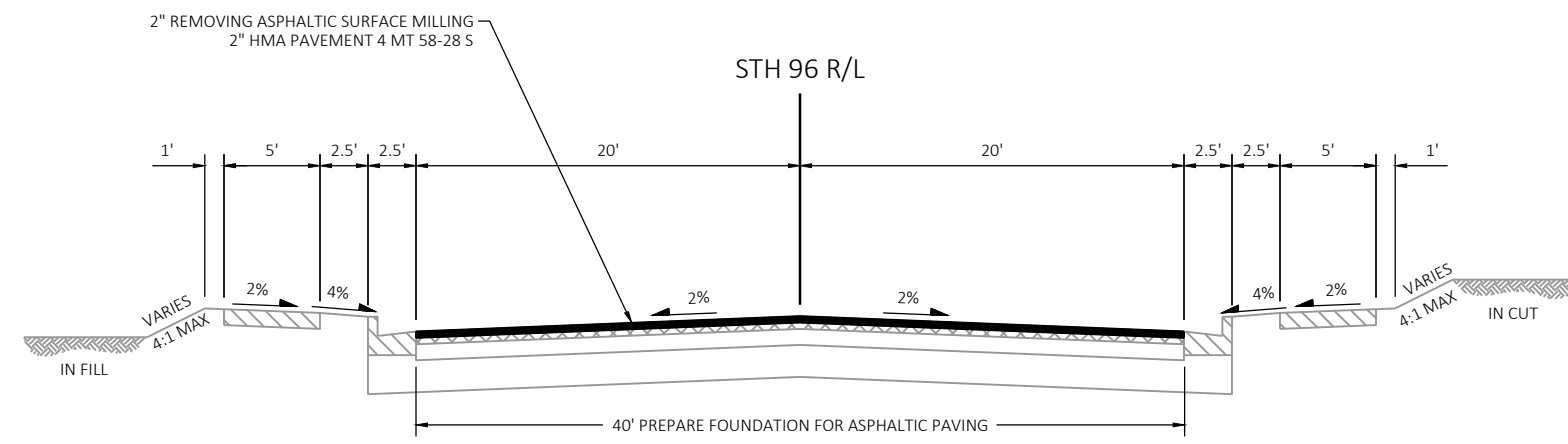
STA 163+76 TO STA 176+88
STA 250+07 TO STA 253+25



FINISHED TYPICAL SECTION STH 96

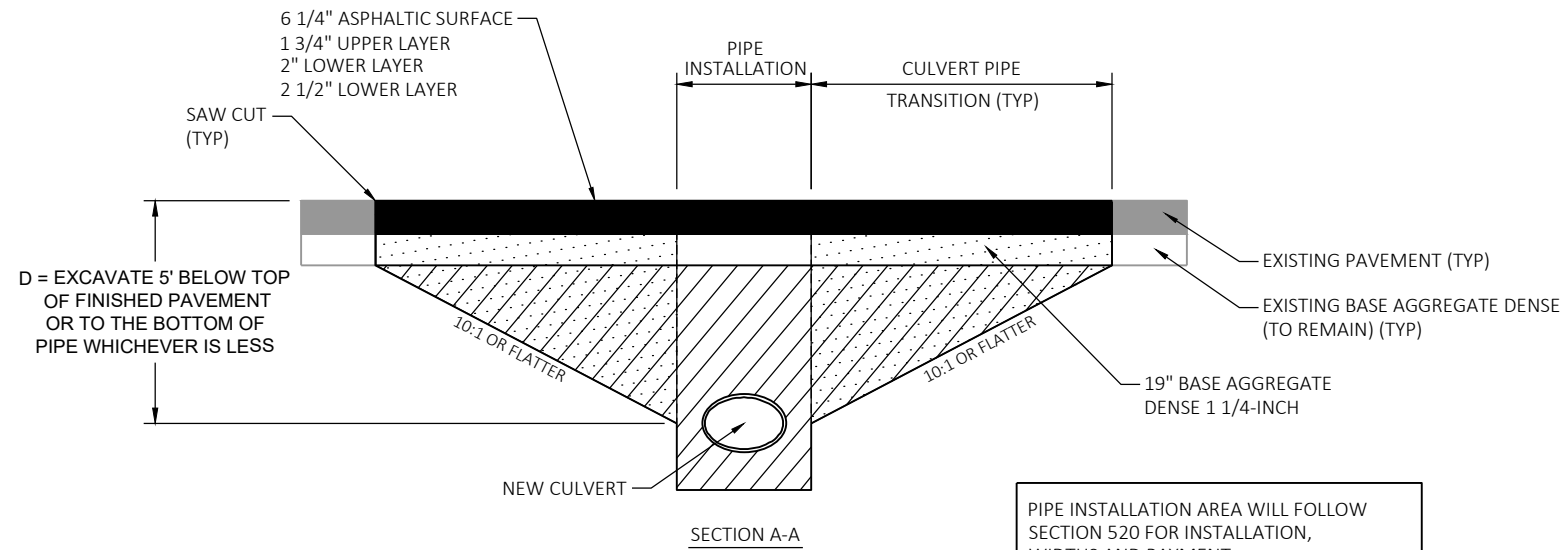
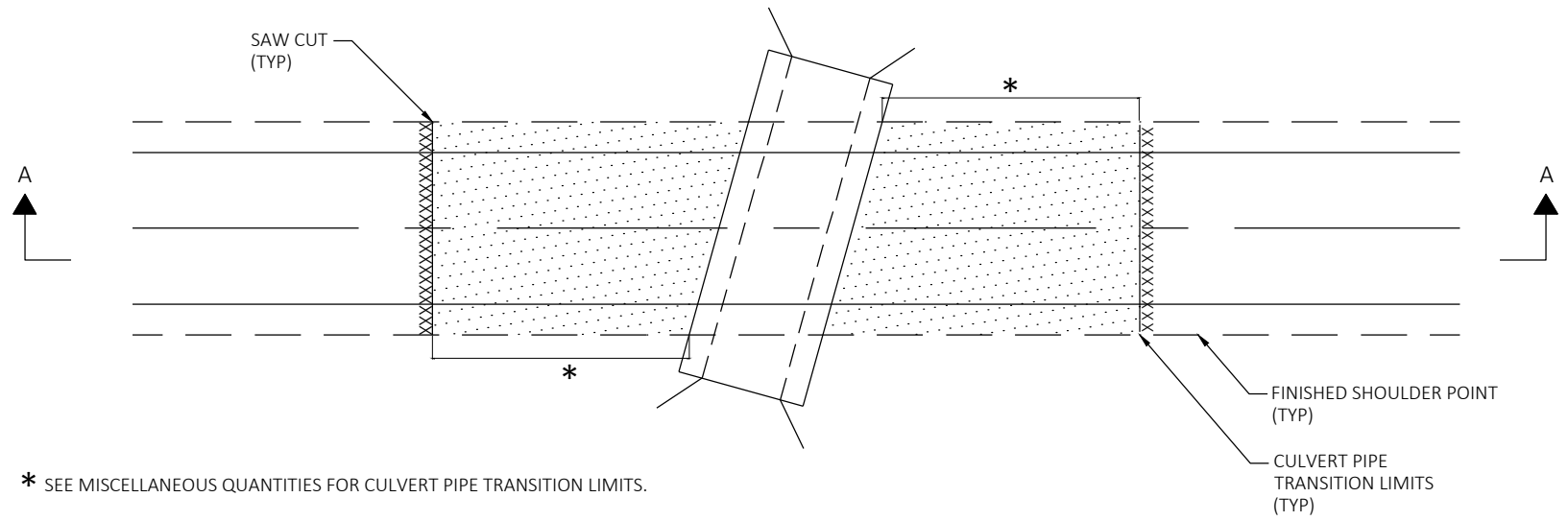
STA 267+29 TO STA 270+84 LT

STA 253+25 TO STA 274+66



FINISHED TYPICAL SECTION STH 96

STA 274+66 TO STA 291+71.36



COMMON EXCAVATION

FOUNDATION BACKFILL

PIPE INSTALLATION AREA WILL FOLLOW SECTION 520 FOR INSTALLATION, WIDTHS AND PAYMENT.

CONSTRUCT TRANSITION PERPENDICULAR TO CULVERT PIPE.

CULVERT PIPE TRANSITION AREAS WILL BE PAID BY COMMON EXCAVATION & SPV FOUNDATION BACKFILL.

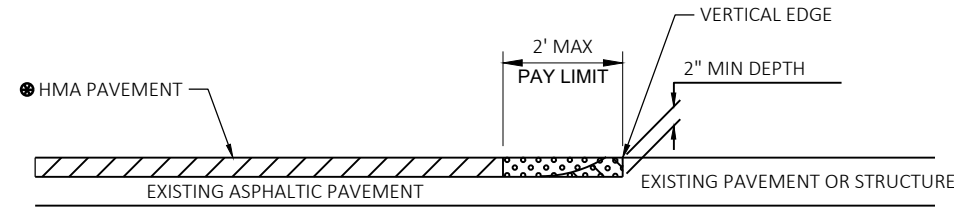
PAVEMENT SAW CUT TO BE PERPENDICULAR TO ROADWAY ALIGNMENT.




PLACE 6 1/4" ASPHALTIC SURFACE PRIOR TO OPENING TO TRAFFIC.

TOP 2" TO BE MILLED OFF PRIOR TO OVERLAY.

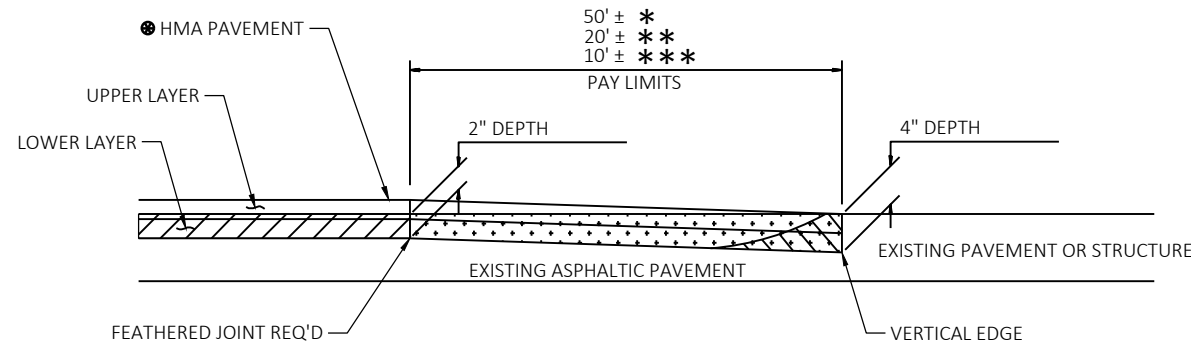
NEW CULVERT PIPES WITH TRANSITION




STA. 118+88.4
STA. 164+07.8
STA. 178+35.5
STA. 217+54.3



- SEE TYPICAL CROSS SECTION FOR PAVEMENT TYPE AND THICKNESS OF INDIVIDUAL LAYERS
-  REMOVING ASPHALTIC SURFACE, MILLING
-  REMOVING ASPHALTIC SURFACE, BUTT JOINTS
-  REMOVE ASPHALTIC SURFACE WEDGE AT BUTT JOINT TO CREATE VERTICAL EDGE

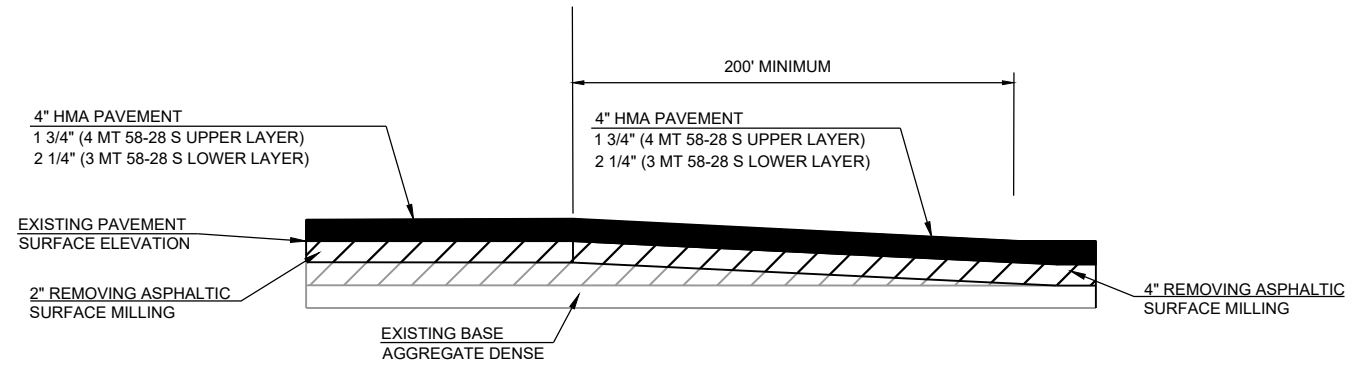
BUTT JOINT DETAIL FOR ASPHALTIC PAVEMENTS (NO PROFILE CHANGE)



- SEE TYPICAL CROSS SECTION FOR PAVEMENT TYPE AND THICKNESS OF INDIVIDUAL LAYERS
-  REMOVING ASPHALTIC SURFACE, MILLING
-  REMOVING ASPHALTIC SURFACE, BUTT JOINTS
-  REMOVE ASPHALTIC SURFACE WEDGE AT BUTT JOINT TO CREATE VERTICAL EDGE

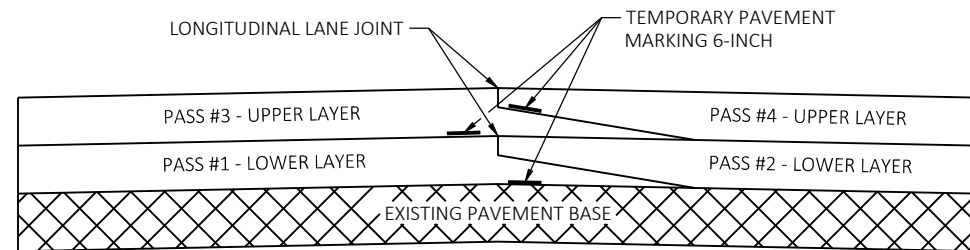
BUTT JOINT DETAIL FOR MILLED ASPHALTIC PAVEMENTS (PROFILE CHANGE)

- * MAINLINE
- ** SIDEROADS
- *** PRIVATE ENTRANCES

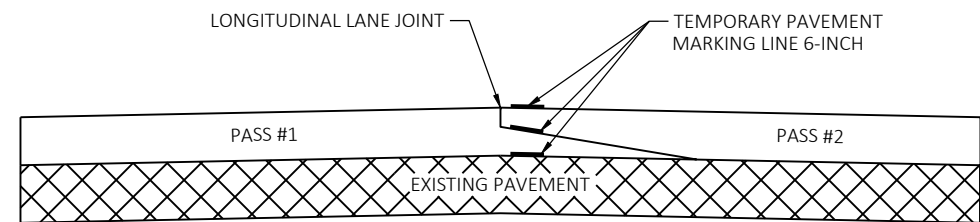


2" TO 4" MILLING TRANSITION DETAIL

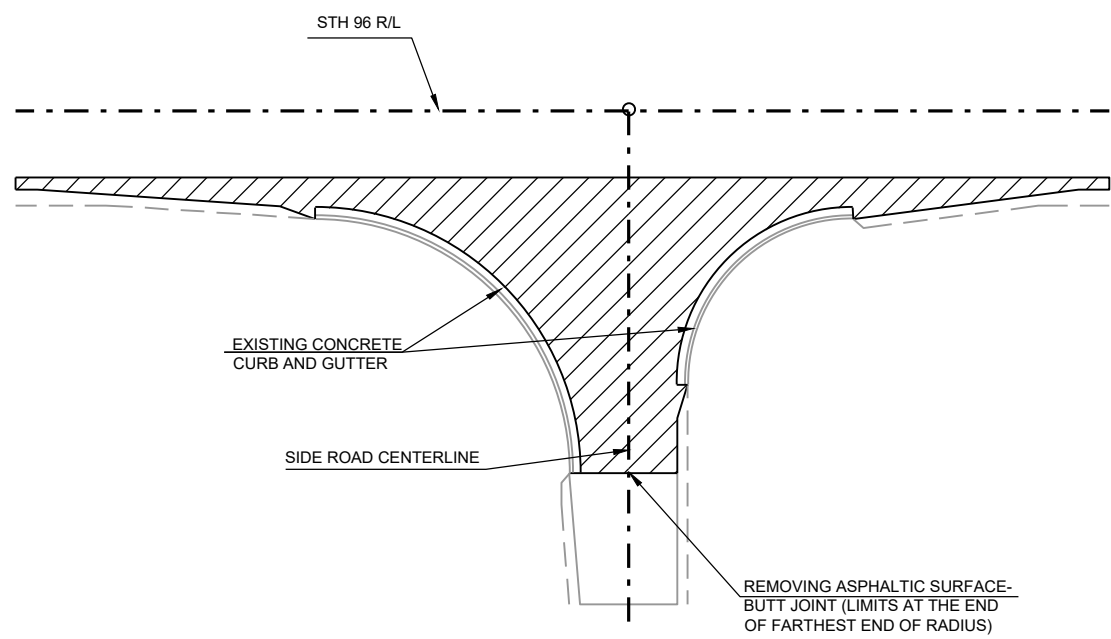
2" MILL / 2" HMA TO 2" MILL / 4" HMA - STA. 127+04 TO STA. 129+04
 2" MILL / 4" HMA TO 4" MILL / 4" HMA - STA. 134+33 TO STA. 136+33
 4" MILL / 4" HMA TO 2" MILL / 4" HMA - STA. 139+99 TO STA. 141+99
 2" MILL / 4" HMA TO 2" MILL / 2" HMA - STA. 251+25 TO STA. 253+25



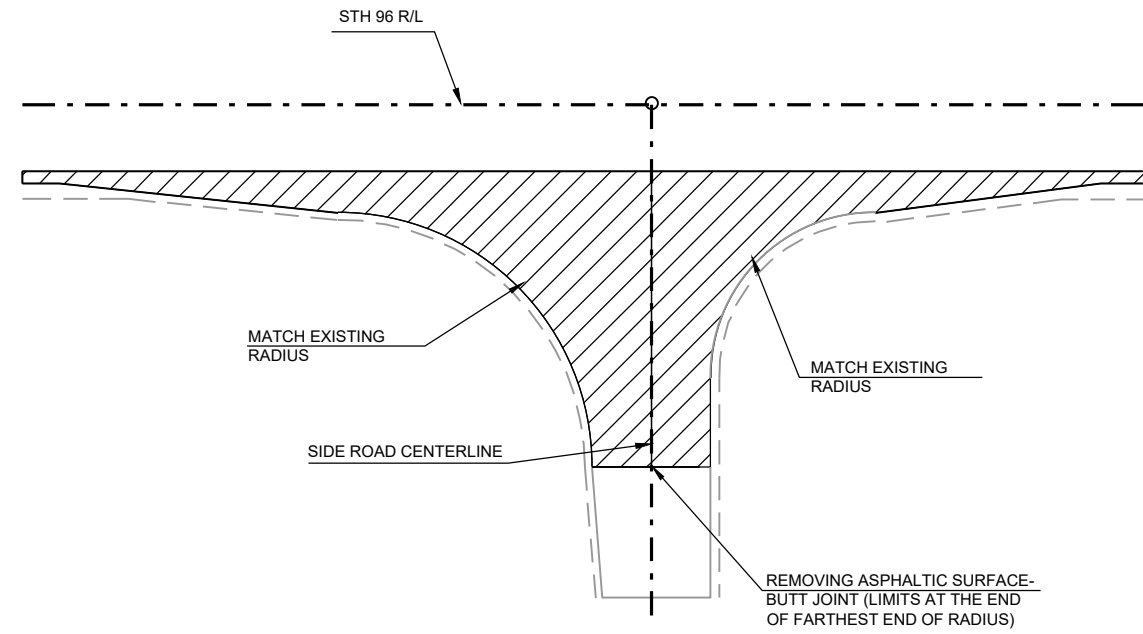
PAVEMENT MARKING DETAIL FOR TAPERED OVERLAPPING JOINTS IN HMA PAVEMENTS



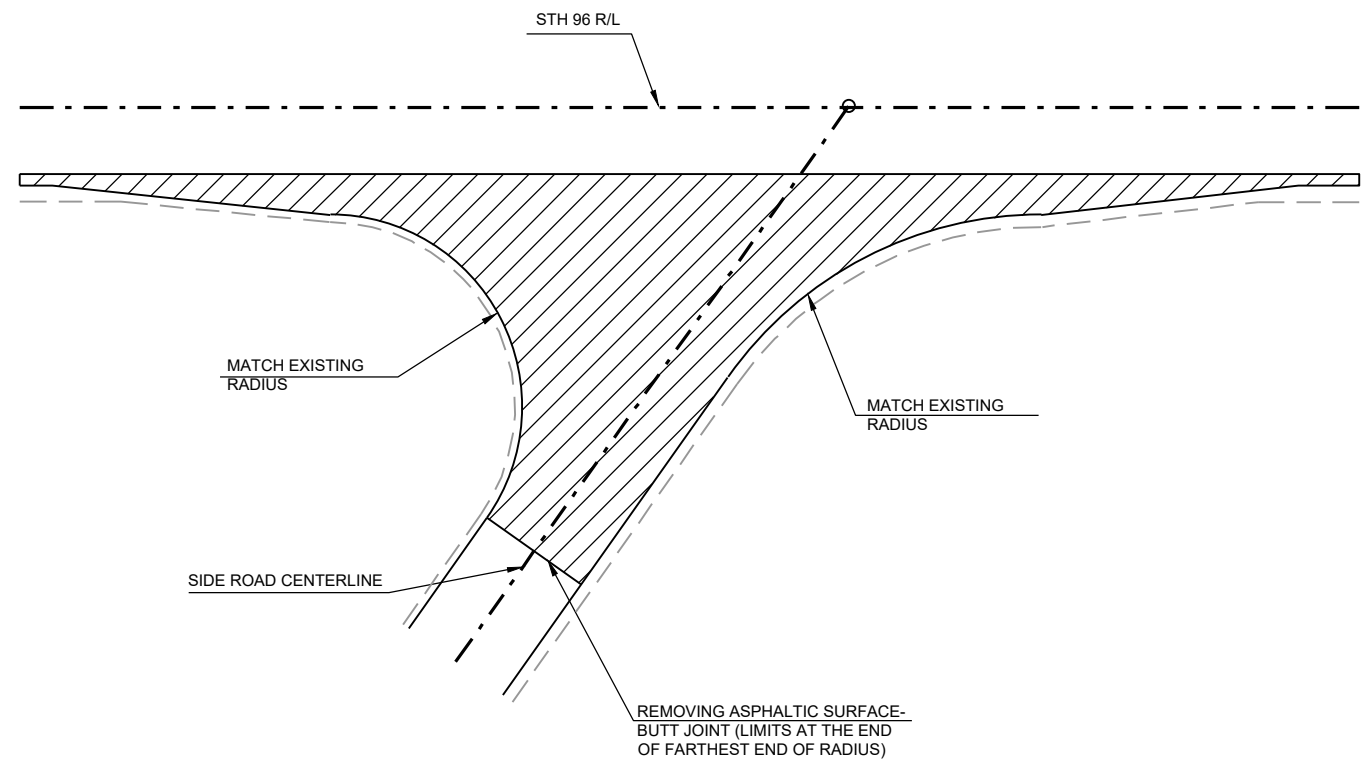
PAVEMENT MARKING DETAIL FOR TAPERED OVERLAPPING JOINTS IN HMA PAVEMENTS



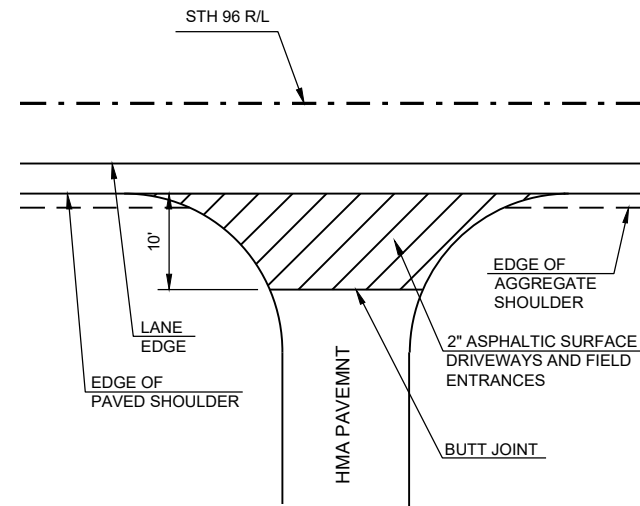
SIDE ROAD CONSTRUCTION LIMITS
 INTERSECTION WITH CONCRETE CURB AND GUTTER



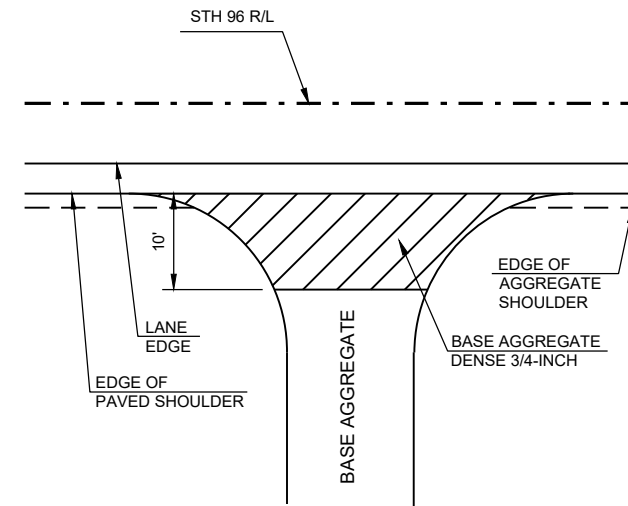
SIDE ROAD CONSTRUCTION LIMITS
 INTERSECTION WITHOUT CONCRETE CURB AND GUTTER



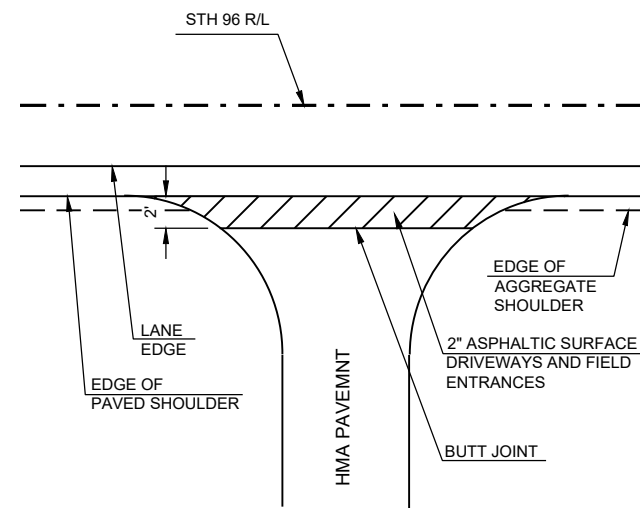
SIDE ROAD CONSTRUCTION LIMITS
 SKEWED SIDE ROAD LOCATIONS



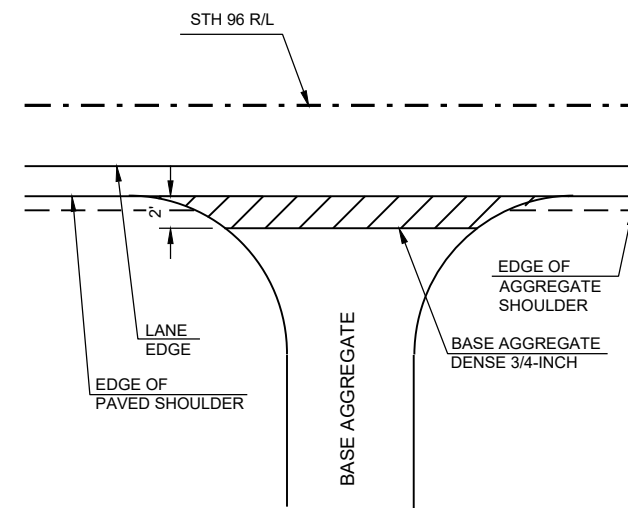
PRIVATE ENTRANCE HMA DETAIL - PROFILE CHANGE



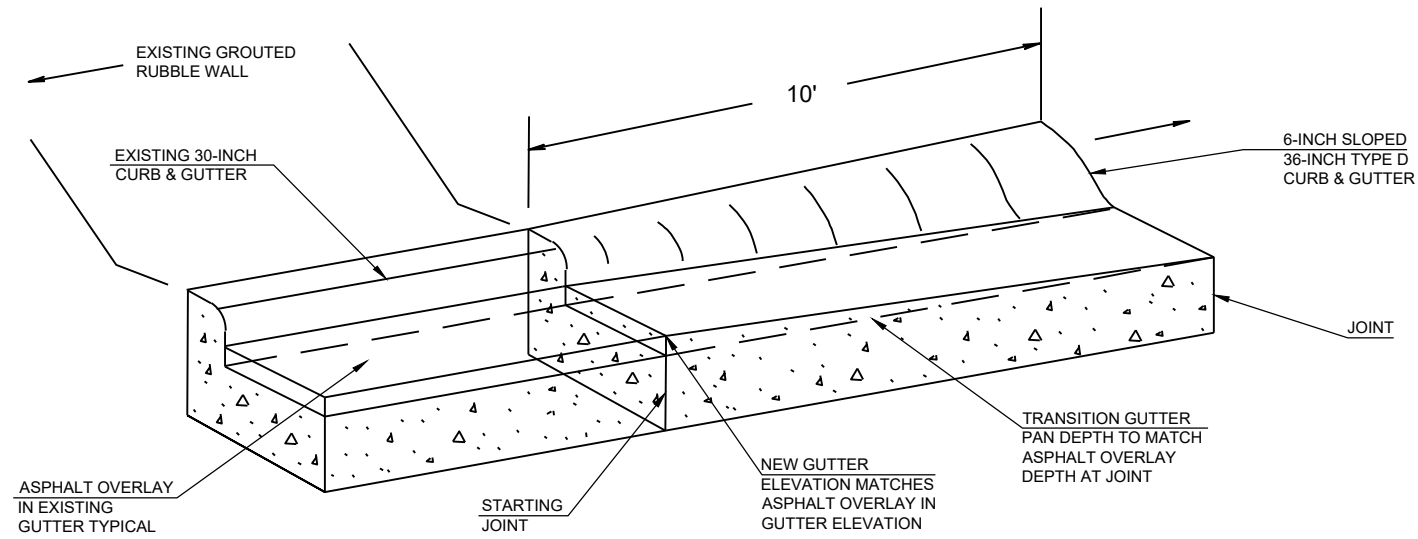
PRIVATE ENTRANCE BASE AGGREGATE DETAIL - PROFILE CHANGE



PRIVATE ENTRANCE HMA DETAIL - NO PROFILE CHANGE



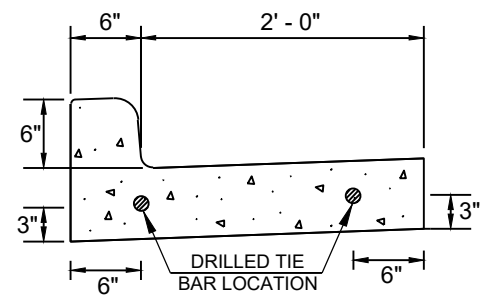
PRIVATE ENTRANCE BASE AGGREGATE DETAIL - NO PROFILE CHANGE



TRANSITION DETAIL

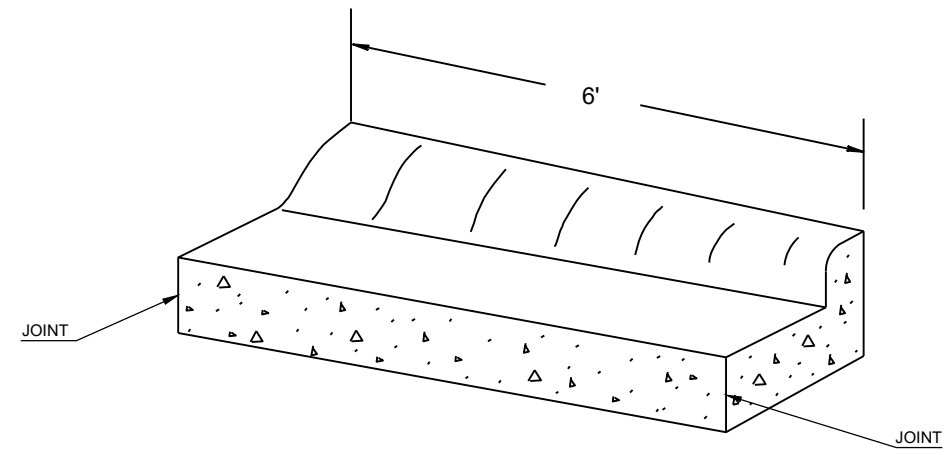
FROM ASPHALT OVERLAY IN GUTTER PAN TO GUTTER PAN SHOWING
(TO BE MEASURED & PAID FOR AS 6-INCH SLOPED 36" CONC. C&G TYPE D)

APPROX STA. 136+33 RT



DRILLED TIE BAR LOCATION FOR MATCHING INTO EXISTING CURB AND GUTTER

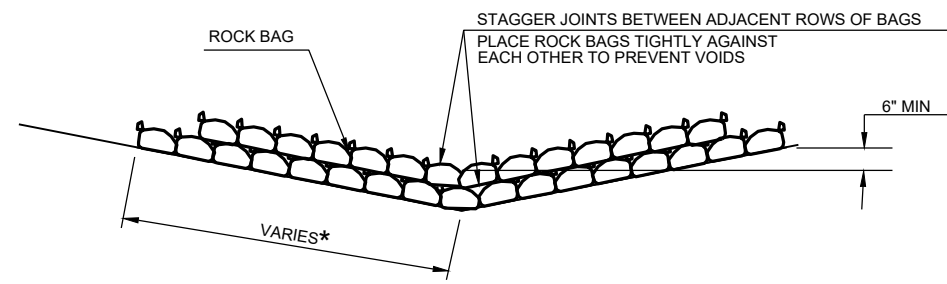
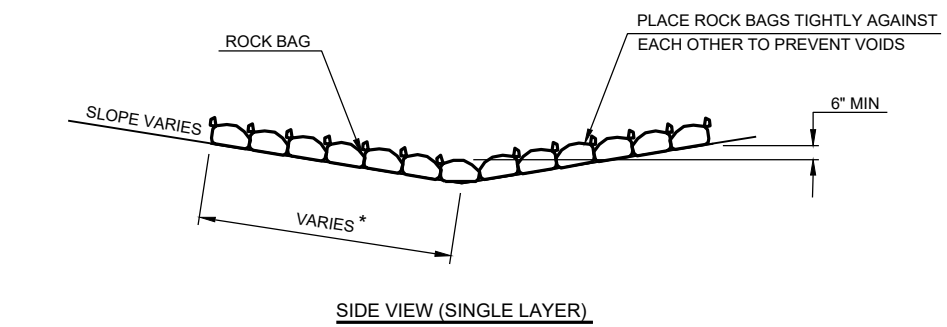
USE AT ALL CURB AND GUTTER REPLACEMENT LOCATIONS



TRANSITION DETAIL

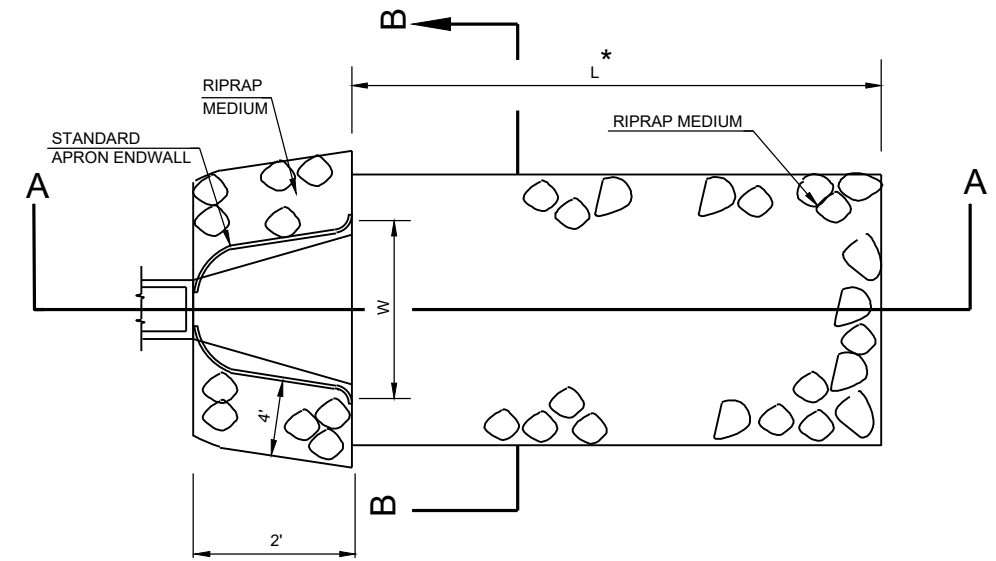
36" TYPE "D" CURB & GUTTER TO 30" TYPE "D" CURB & GUTTER
(TO BE MEASURED & PAID FOR AS 36" CONC. C&G)

LAMP LIGHTER DR

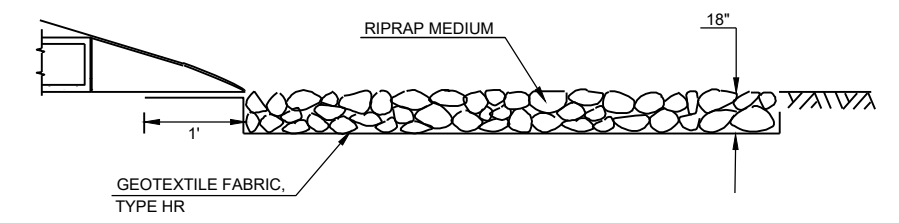


LENGTH AND NUMBER OF BAGS MAY VARY
* DEPENDING ON DESIRED DEPTH OF WATER POOL

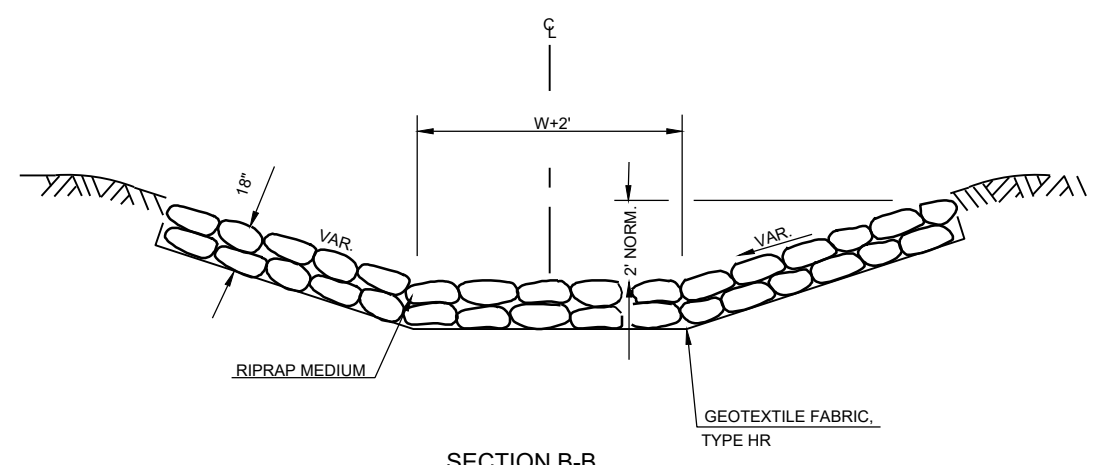
ROCK BAGS DITCH CHECK
PAID AS ROCK BAGS
(SEE MISCELLANEOUS QUANTITIES FOR LOCATIONS)



* L = 3 TIMES DIAMETER (NORMAL) OR
10' MIN. OR AS DIRECTED BY THE
ENGINEER

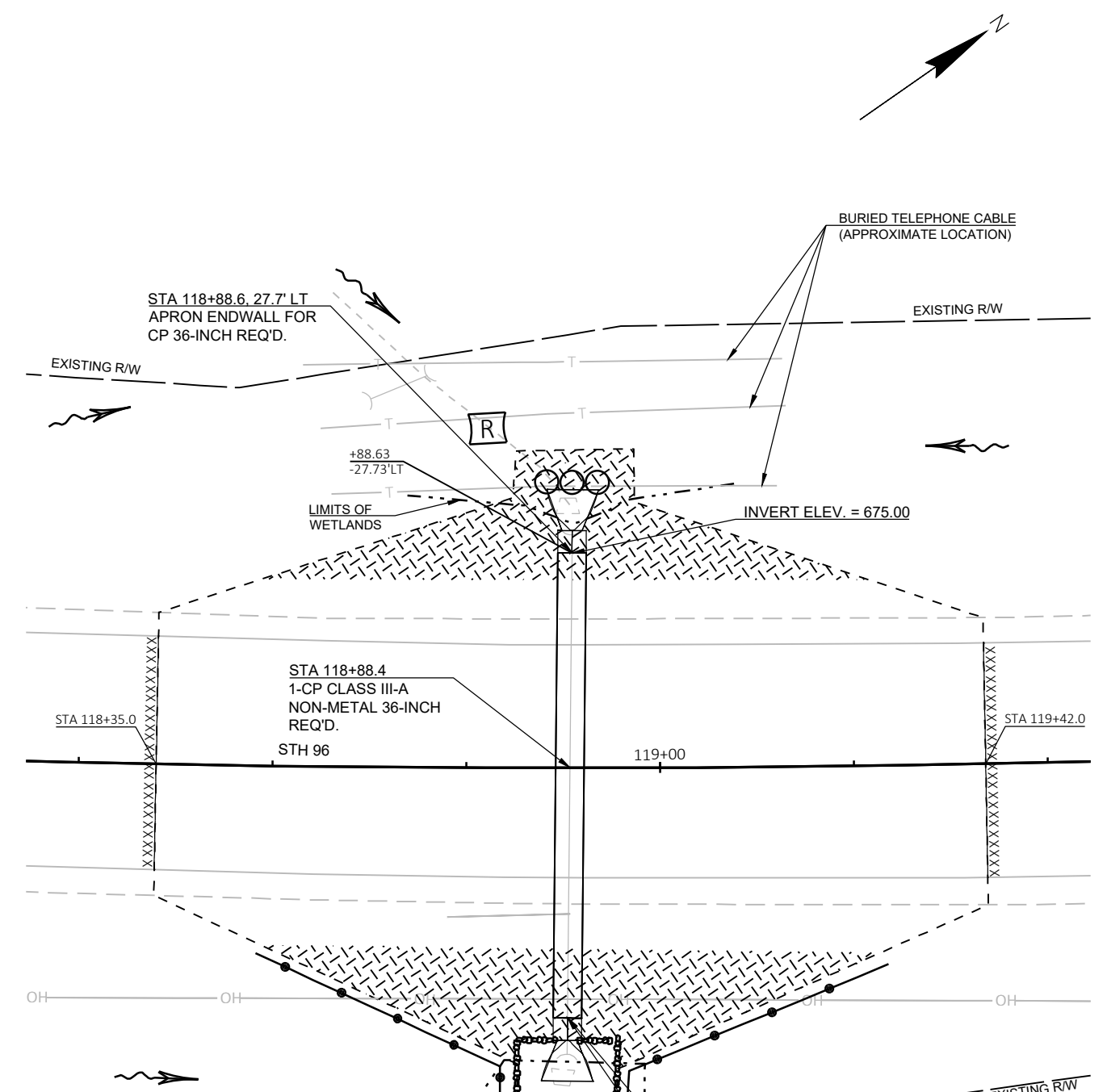
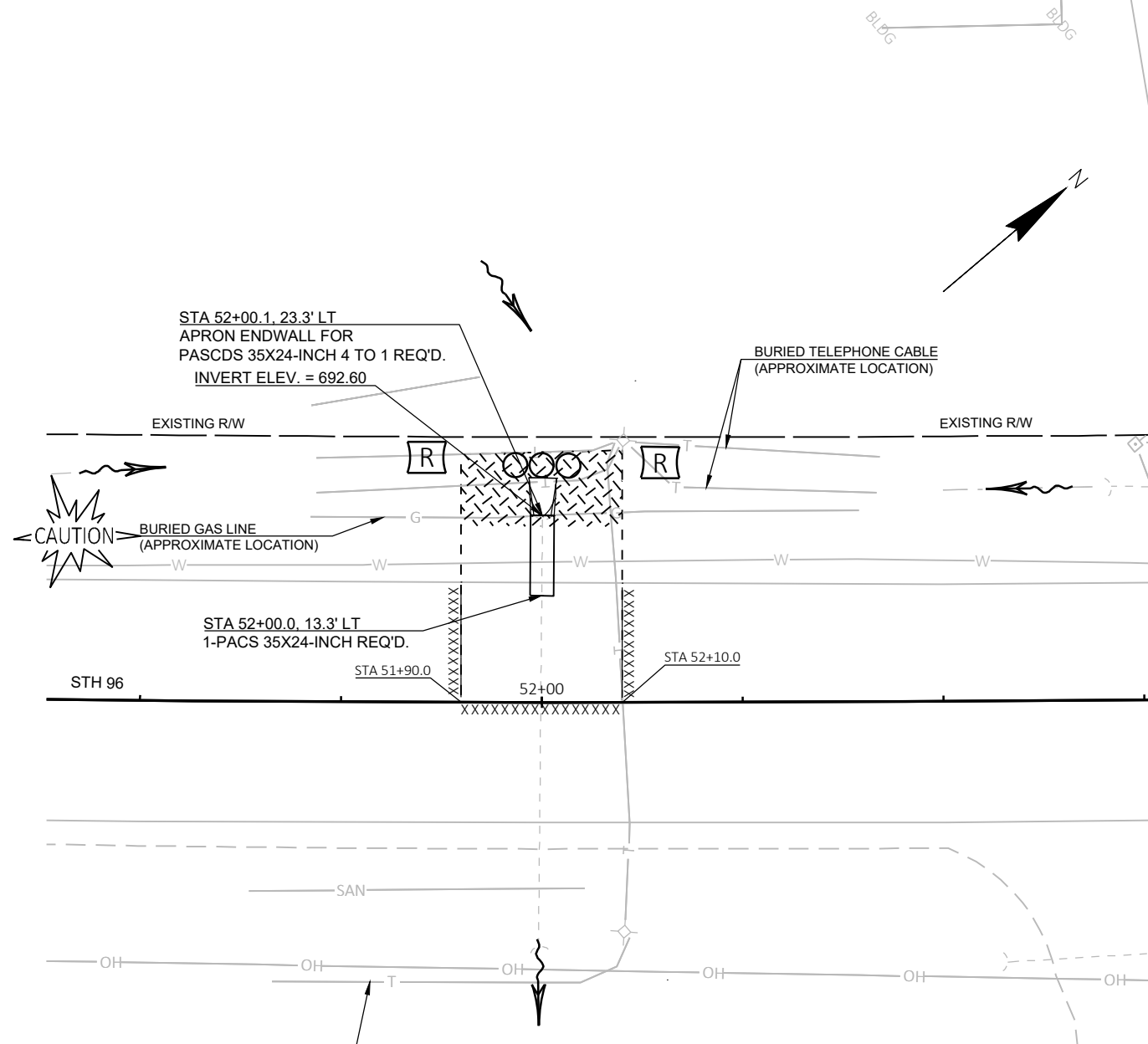


SECTION A-A



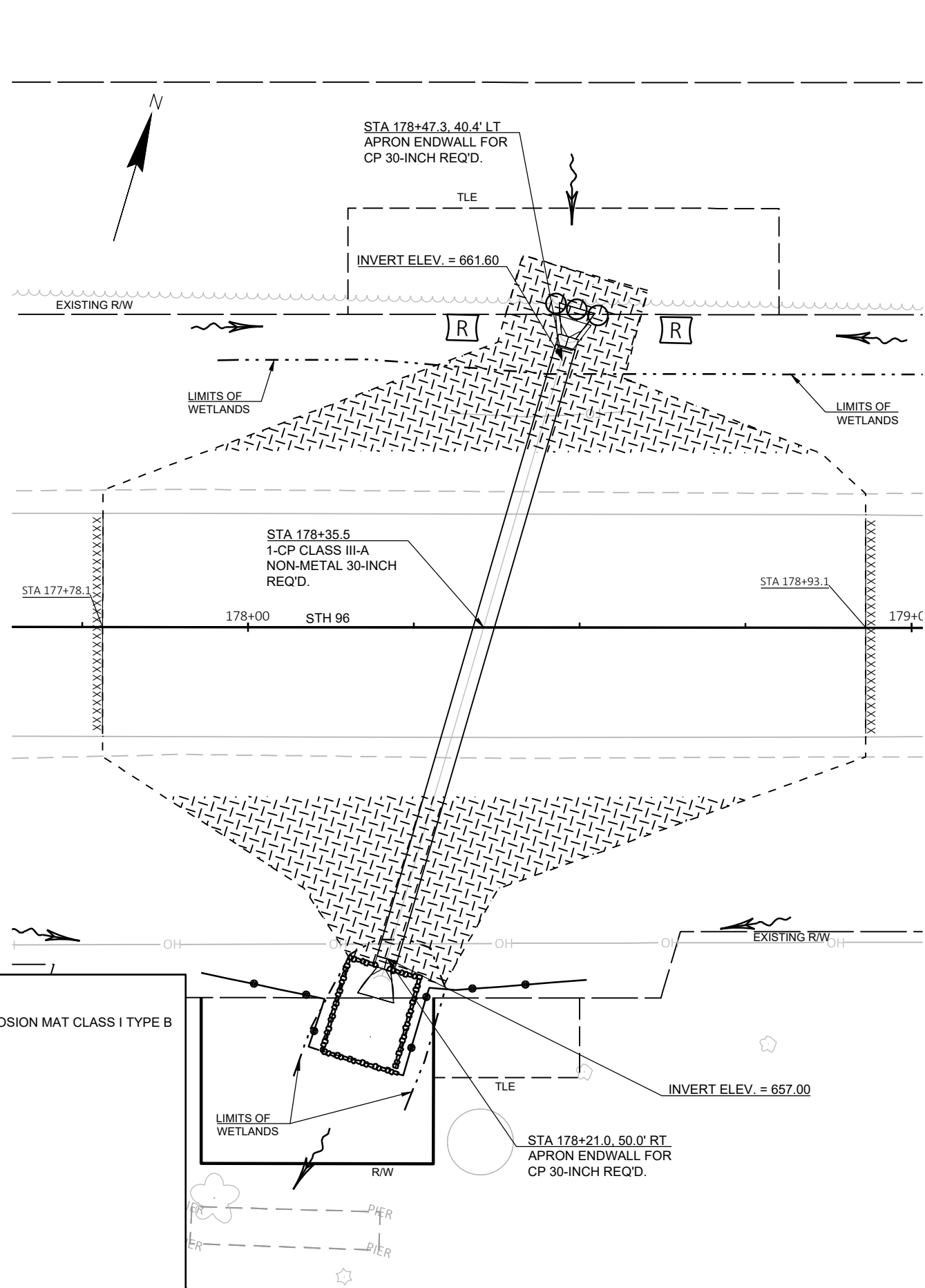
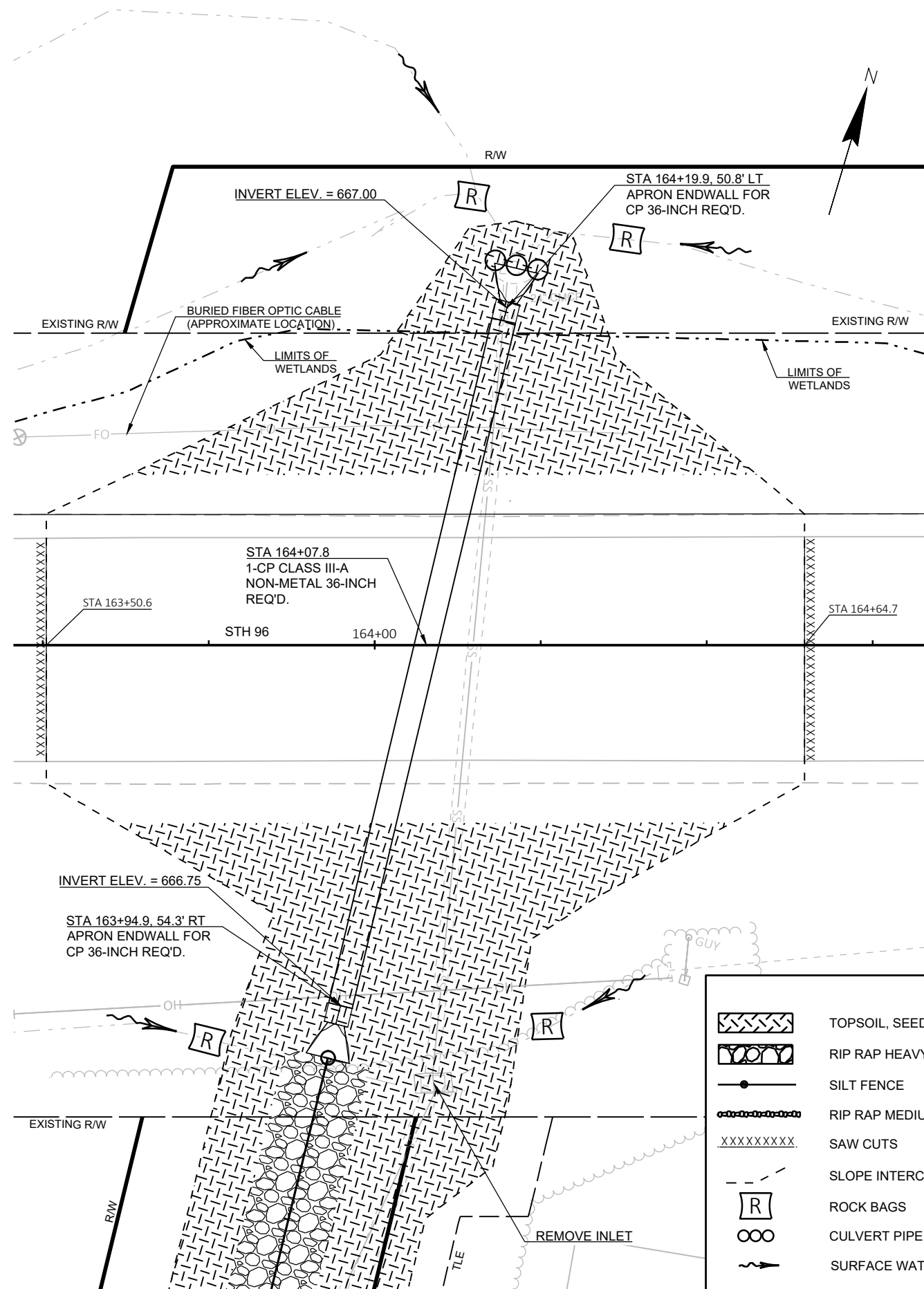
SECTION B-B

**RIPRAP MEDIUM AND GEOTEXTILE FABRIC
DETAIL AT APRON ENDWALLS**
(SEE MISCELLANEOUS QUANTITIES FOR LOCATIONS)



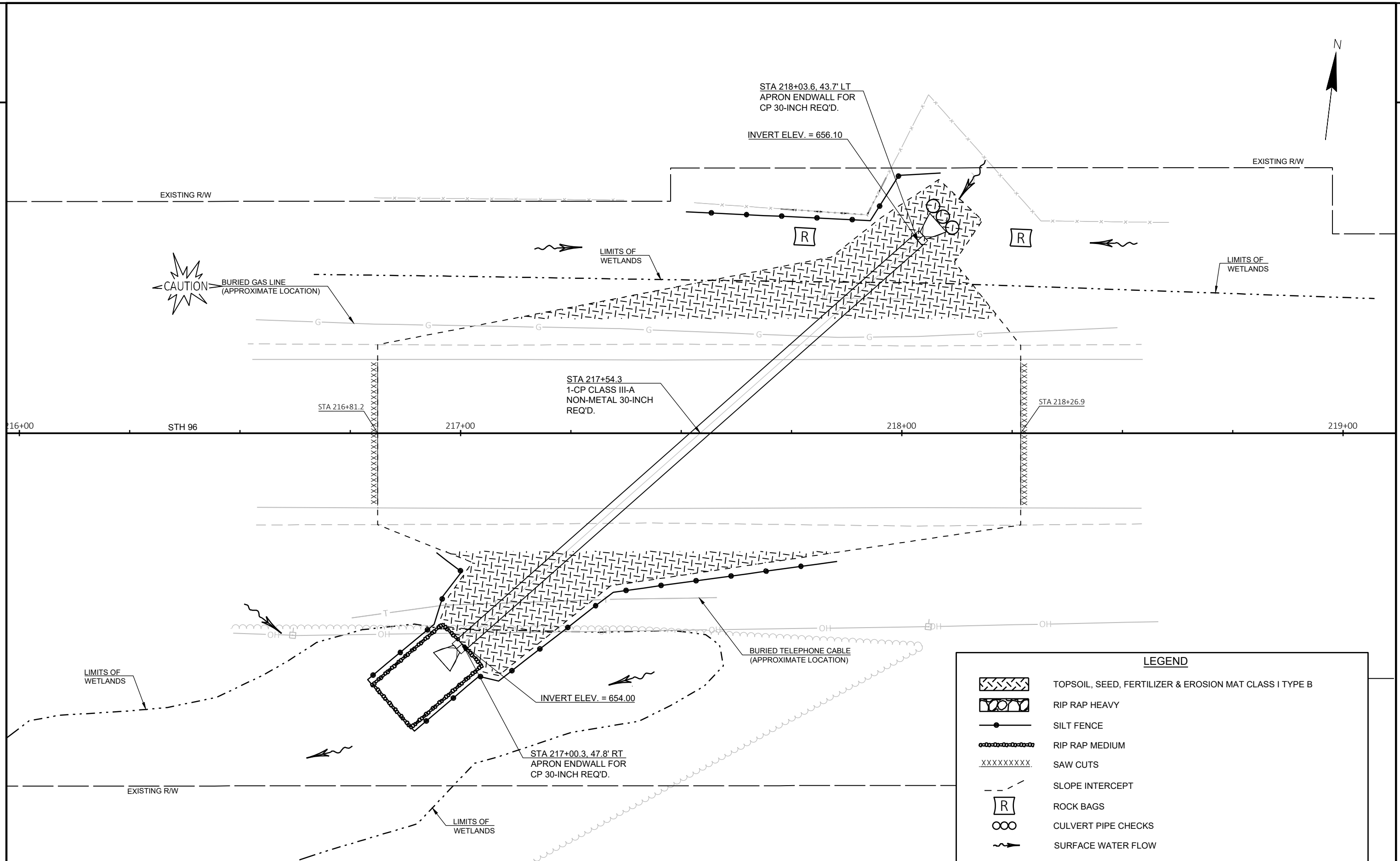
LEGEND

	TOPSOIL, SEED, FERTILIZER & EROSION MAT CLASS I TYPE B
	RIP RAP HEAVY
	RIP RAP MEDIUM
	SAW CUTS
	SLOPE INTERCEPT
	ROCK BAGS
	CULVERT PIPE CHECKS
	SURFACE WATER FLOW






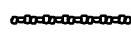
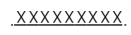
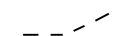
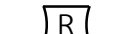


LEGEND

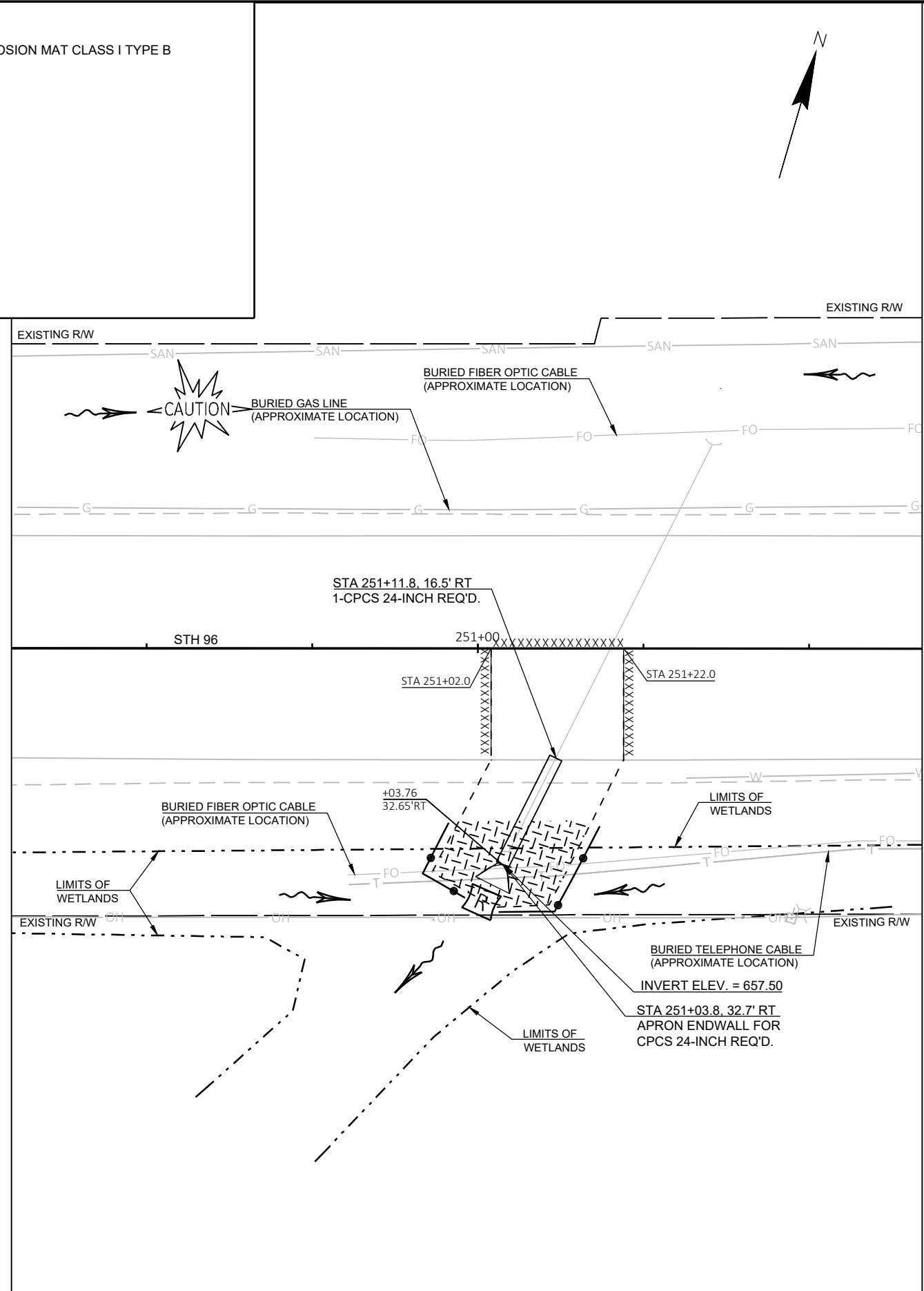
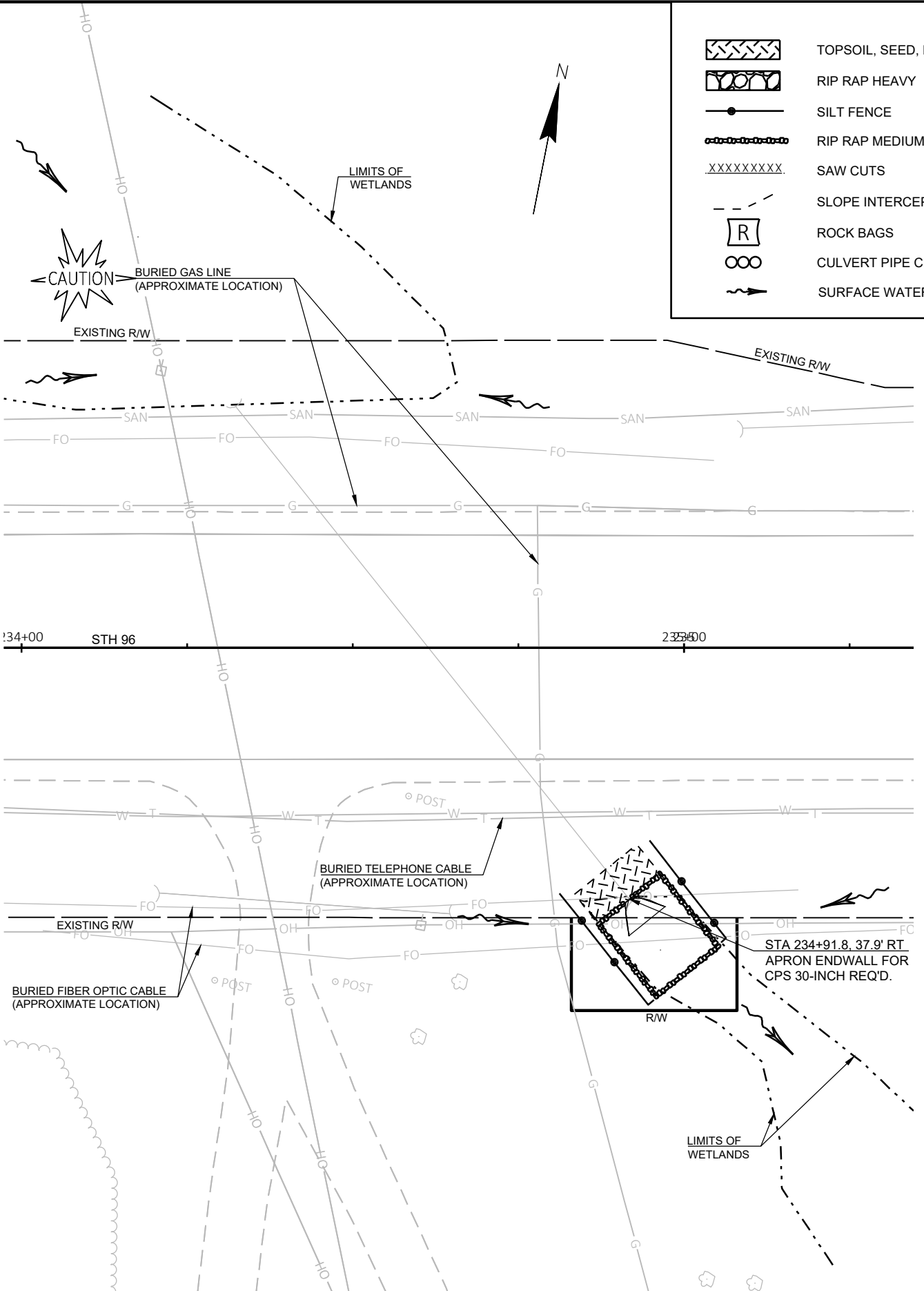
	TOPSOIL, SEED, FERTILIZER & EROSION MAT CLASS I TYPE B
	RIP RAP HEAVY
	SILT FENCE
	RIP RAP MEDIUM
	SAW CUTS
	SLOPE INTERCEPT
	ROCK BAGS
	CULVERT PIPE CHECKS
	SURFACE WATER FLOW

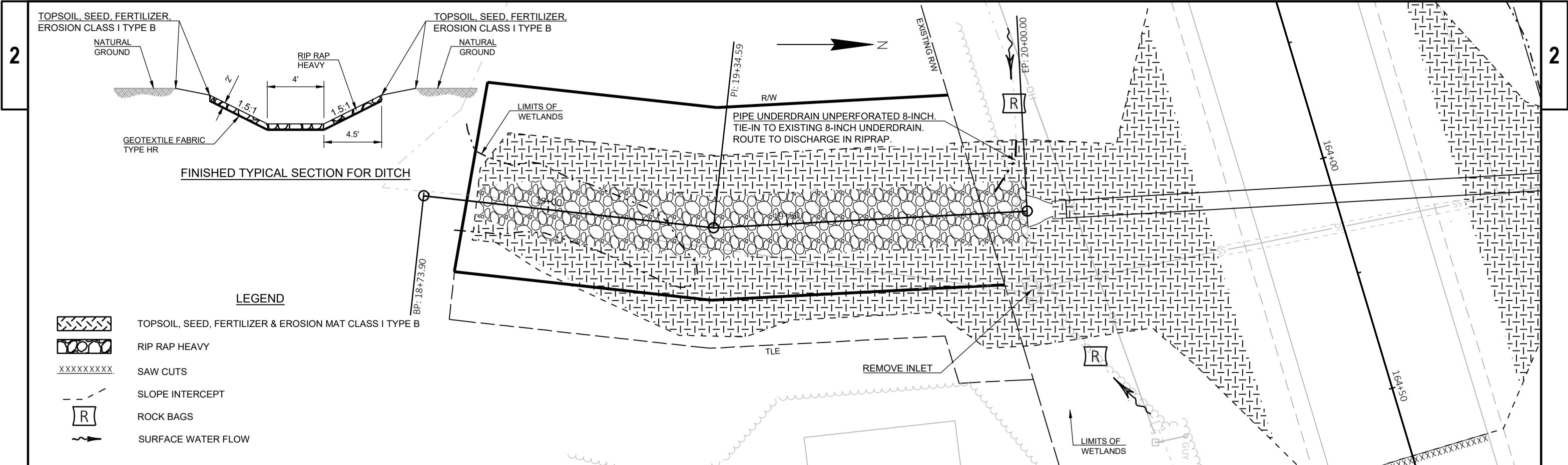


LEGEND	
	TOPSOIL, SEED, FERTILIZER & EROSION MAT CLASS I TYPE B
	RIP RAP HEAVY
	SILT FENCE
	RIP RAP MEDIUM
	SAW CUTS
	SLOPE INTERCEPT
	ROCK BAGS
	CULVERT PIPE CHECKS
	SURFACE WATER FLOW

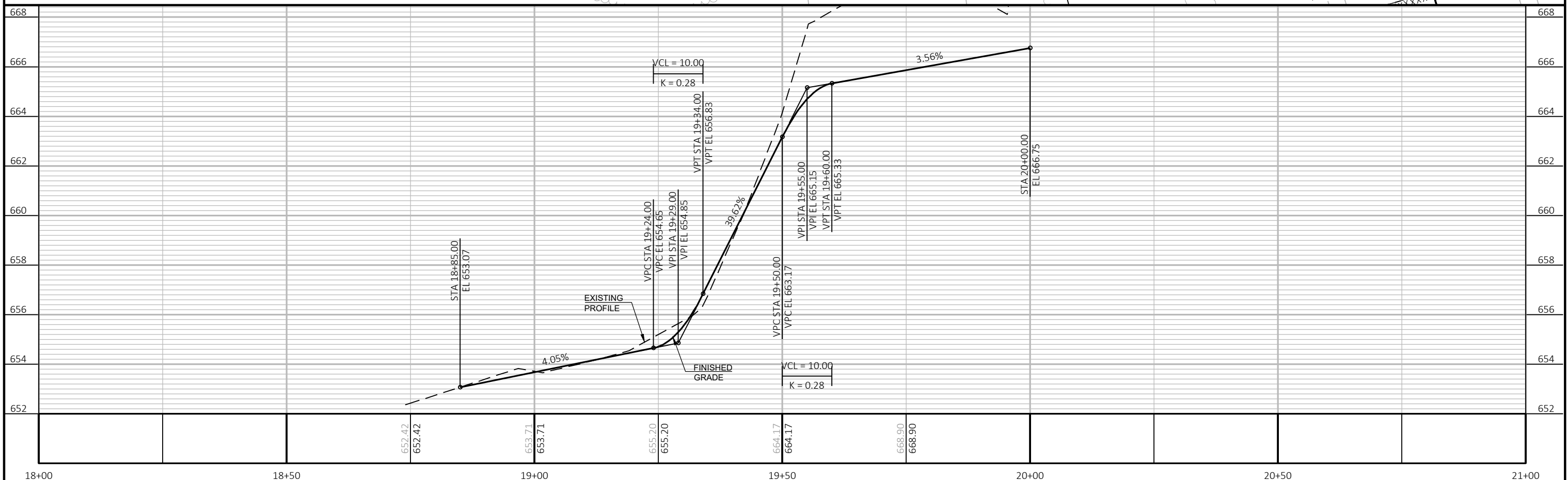
LEGEND

-  TOPSOIL, SEED, FERTILIZER & EROSION MAT CLASS I TYPE B
-  RIP RAP HEAVY
-  SILT FENCE
-  RIP RAP MEDIUM
-  SAW CUTS
-  SLOPE INTERCEPT
-  ROCK BAGS
-  CULVERT PIPE CHECKS
-  SURFACE WATER FLOW



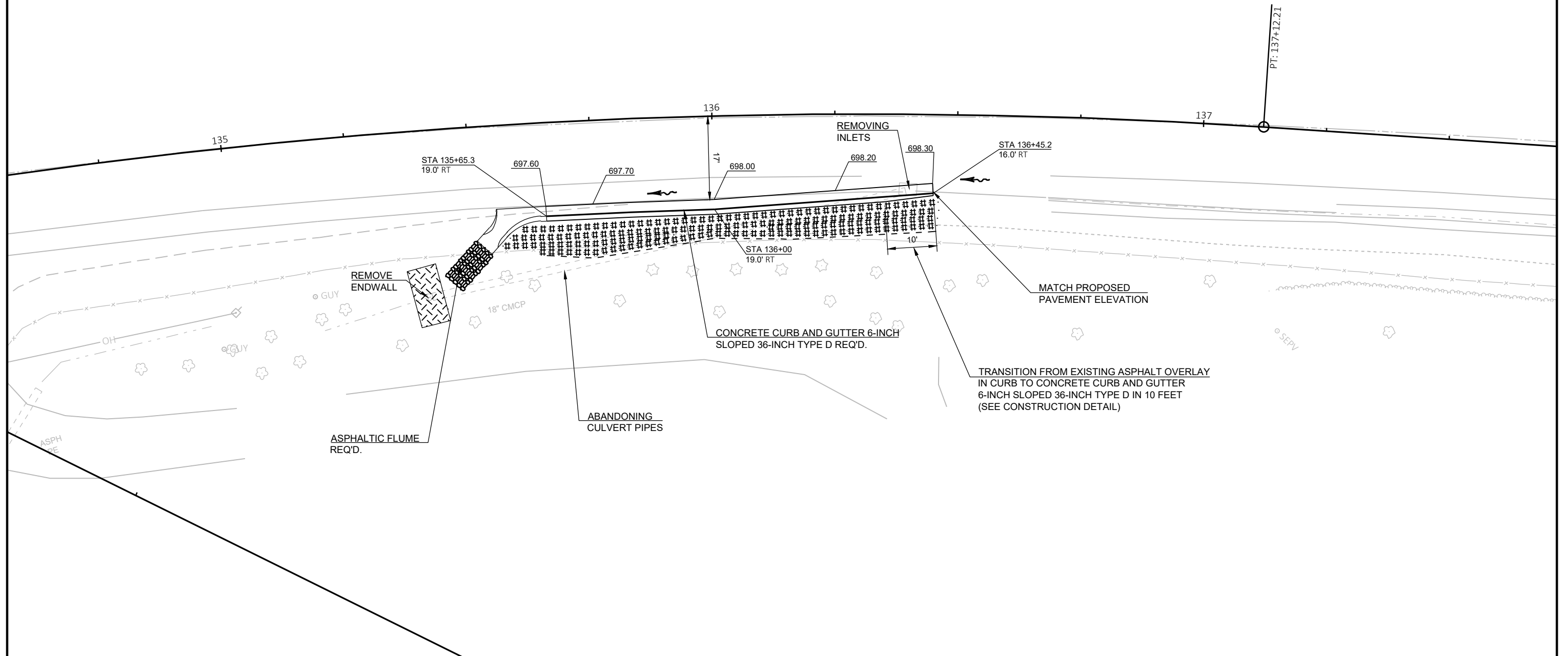
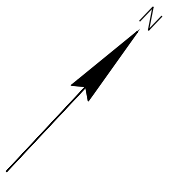


- LEGEND**
- TOPSOIL, SEED, FERTILIZER & EROSION MAT CLASS I TYPE B
 - RIP RAP HEAVY
 - SAW CUTS
 - SLOPE INTERCEPT
 - ROCK BAGS
 - SURFACE WATER FLOW



LEGEND

- ##### EROSION MAT URBAN CLASS I, TYPE B
- ▨▨▨▨▨ EROSION MAT CLASS I, TYPE B
- ▩▩▩▩▩ RIPRAP MEDIUM
- - - SLOPE INTERCEPT
- SURFACE WATER FLOW

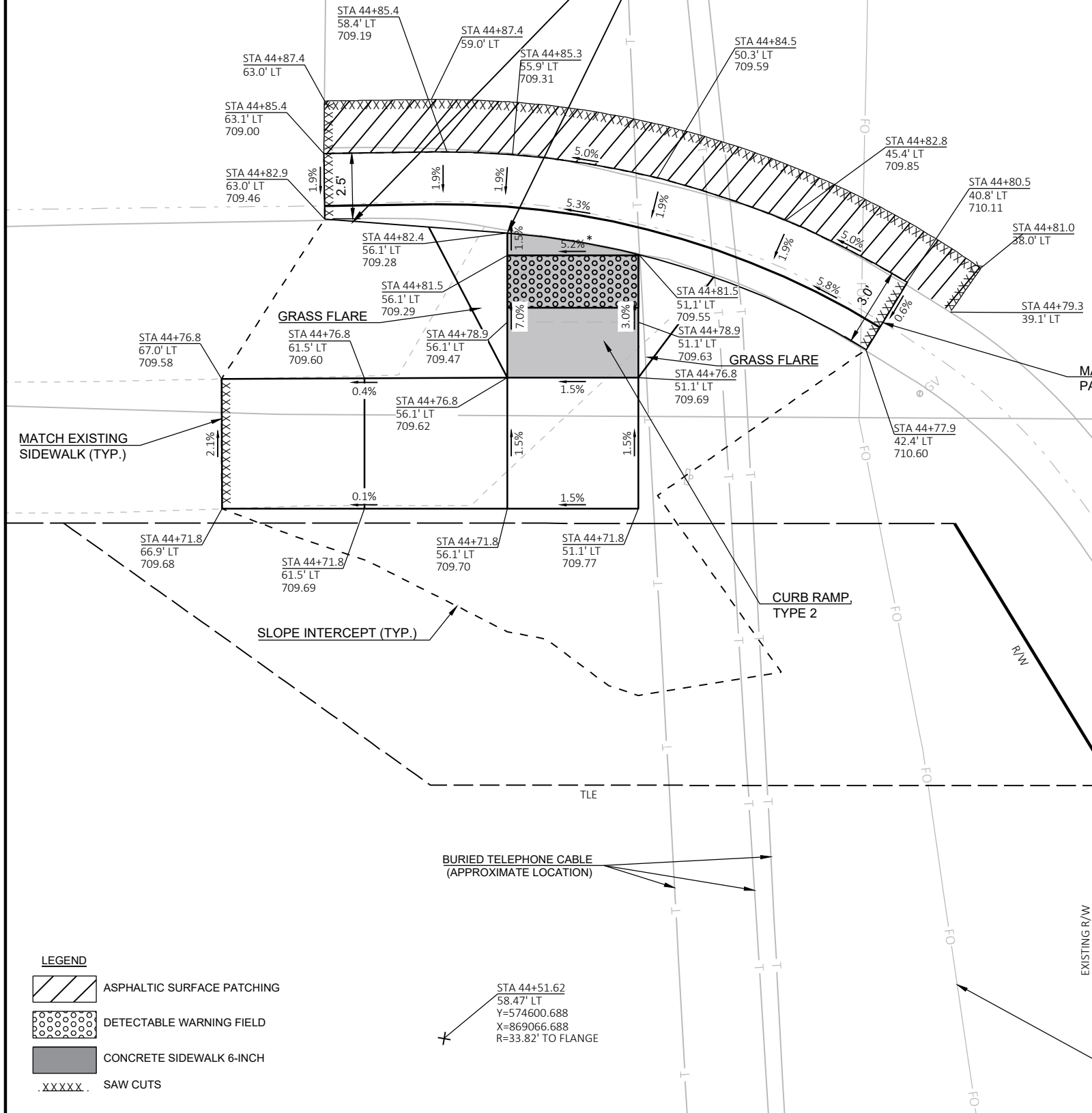
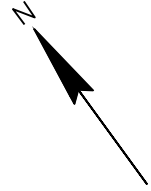


LAMP LIGHTER DR

TRANSITION BETWEEN
CONCRETE CURB & GUTTER 30-INCH TYPE D
CONCRETE CURB & GUTTER 6-INCH SLOPED 36-INCH TYPE D

NOTE:
 -CONTRACTOR TO FIELD VERIFY, GRADES, SLOPES, LENGTHS, AND MATCH POINTS PRIOR TO CURB RAMP AND SIDEWALK CONSTRUCTION
 -THE ENGINEER MAY ADJUST ELEVATIONS TO FIELD FIT CONSTRUCTION
 -ASPHALT SURFACE PATCHING LIMITS ARE 2' TYPICAL OUTSIDE THE LIMITS OF THE CURB & GUTTER REMOVAL
 -A TECHNICALLY INFEASIBLE REPORT (TIF) WAS COMPLETED FOR CURB RAMP ELEMENTS NOT MEETING STANDARDS. SEE ENGINEER FOR ADDITIONAL DETAILS
 -DASHED LINES SHOWN FOR PROPOSED SIDEWALK ARE FOR INFORMATION ONLY AND DO NOT INDICATE JOINT LOCATIONS
 -GRADE CHANGE BETWEEN THE GUTTER FLANGE SLOPE AND CURB RAMP SLOPE SHALL NOT EXCEED 11%
 -ALL SIDEWALK IS CONCRETE SIDEWALK 4-INCH UNLESS NOTED
 -ALL STATIONS AND OFFSETS ARE TO STH 96 REFERENCE LINE
 -ALL CURB RAMP FLARES ARE GRADED GRASS FLARES
 -REFER TO SDD 8D5 "CURB RAMPS" FOR ADDITIONAL DETAILS

*MAXIMUM EXTENT FEASIBLE



R/L STH 96



BURIED GAS LINE
(APPROXIMATE LOCATION)

STH 96

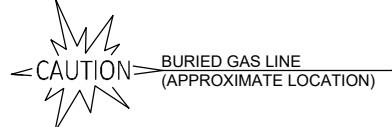
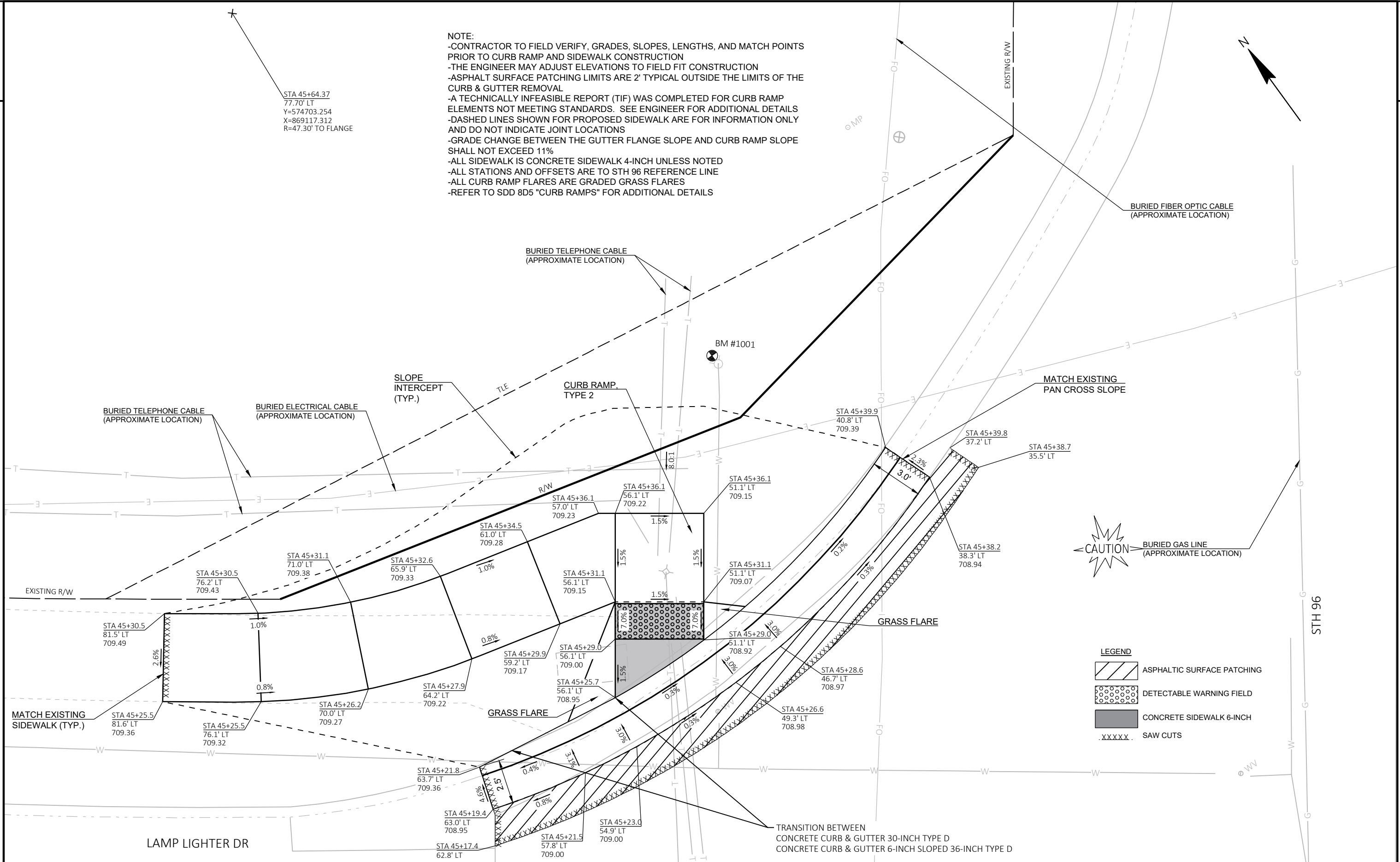
- LEGEND
- ASPHALTIC SURFACE PATCHING
 - DETECTABLE WARNING FIELD
 - CONCRETE SIDEWALK 6-INCH
 - SAW CUTS

STA 44+51.62
 58.47' LT
 Y=574600.688
 X=869066.688
 R=33.82' TO FLANGE

BURIED FIBER OPTIC CABLE
(APPROXIMATE LOCATION)

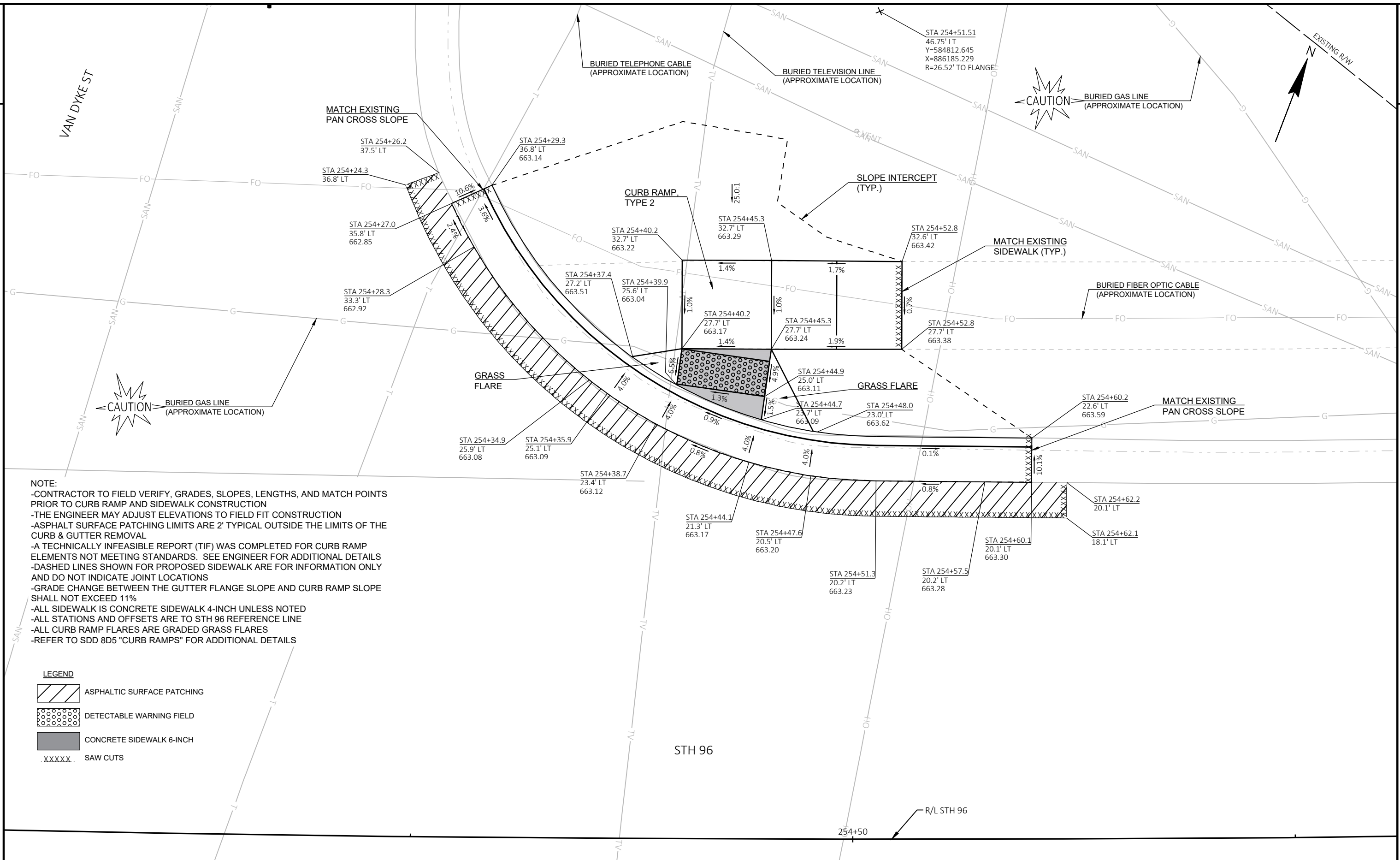
NOTE:
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STA 45+64.37
 77.70' LT
 Y=574703.254
 X=869117.312
 R=47.30' TO FLANGE







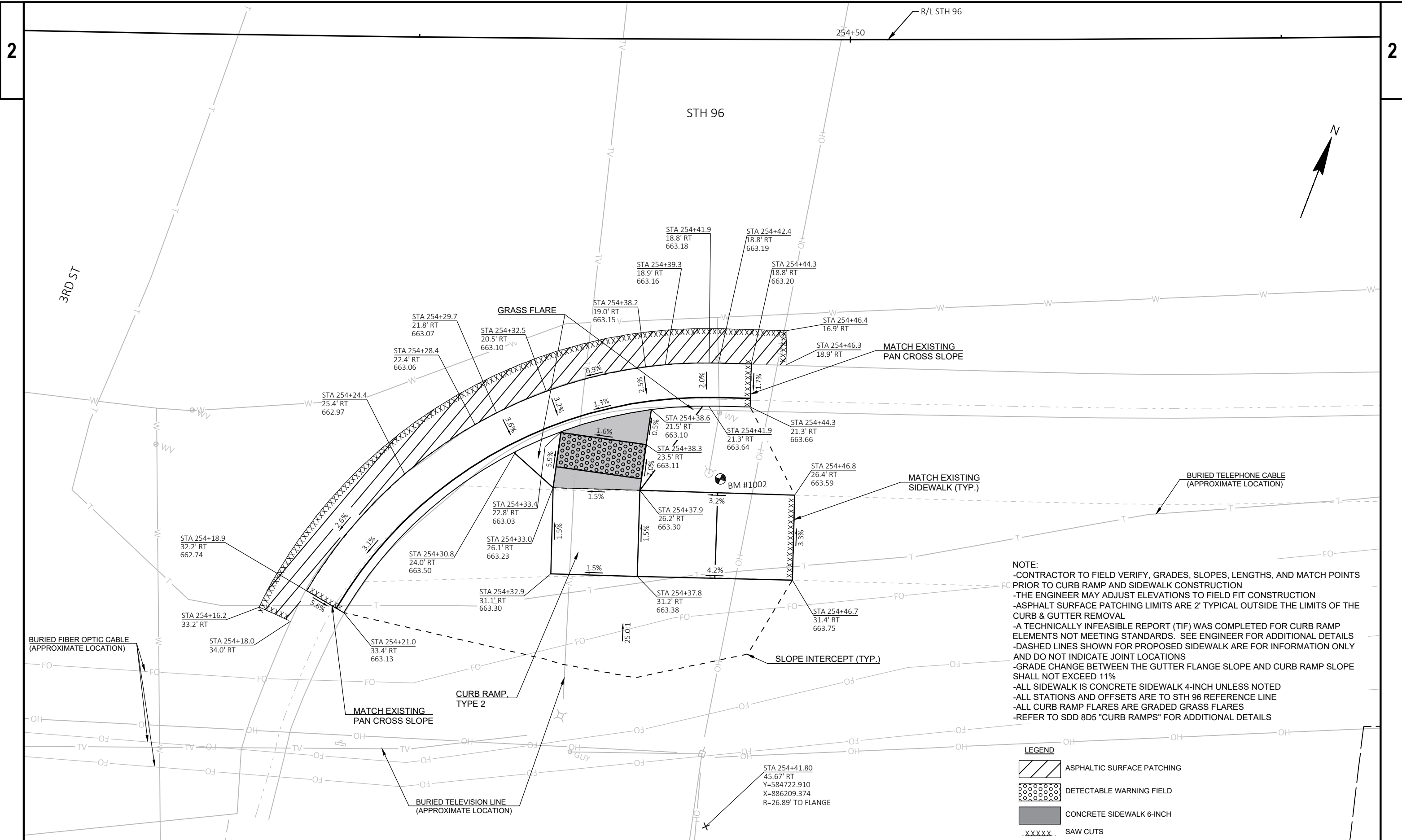
LEGEND

	ASPHALTIC SURFACE PATCHING
	DETECTABLE WARNING FIELD
	CONCRETE SIDEWALK 6-INCH
	SAW CUTS



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- LEGEND**
-  ASPHALTIC SURFACE PATCHING
 -  DETECTABLE WARNING FIELD
 -  CONCRETE SIDEWALK 6-INCH
 -  SAW CUTS







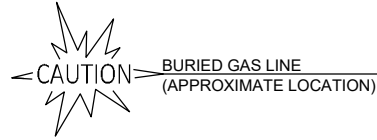
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LEGEND

	ASPHALTIC SURFACE PATCHING
	DETECTABLE WARNING FIELD
	CONCRETE SIDEWALK 6-INCH
	SAW CUTS

- LEGEND**
-  ASPHALTIC SURFACE PATCHING
 -  DETECTABLE WARNING FIELD
 -  CONCRETE SIDEWALK 6-INCH
 -  .XXXXX. SAW CUTS



STA 262+46.23
46.15' LT
Y=585130.430
X=886912.579
R=26.39' TO FLANGE

MATCH EXISTING
PAN CROSS SLOPE

STA 262+72.3
43.9' LT

STA 262+74.3
43.8' LT

STA 262+72.1
41.9' LT
660.47

STA 262+69.6
42.3' LT
660.75

STA 262+67.0
34.6' LT
660.95

STA 262+65.5
32.2' LT
660.47

STA 262+69.2
33.4' LT
660.53

STA 262+67.5
30.7' LT
660.55

STA 262+62.2
25.2' LT
660.61

STA 262+55.0
21.3' LT
660.66

STA 262+49.5
20.0' LT
660.70

STA 262+48.0
19.8' LT
660.71

STA 262+46.5
19.8' LT
660.72

STA 262+39.7
19.7' LT

STA 262+39.7
17.7' LT

STA 262+41.7
19.7' LT
660.67

STA 262+46.0
19.8' LT
660.72

STA 262+43.0
19.7' LT
660.69

CURB RAMP
TYPE 2

STA 262+49.2
32.2' LT
660.89

STA 262+54.2
32.2' LT
660.83

GRASS FLARE
BM #1004

STA 262+60.6
32.2' LT
660.55

STA 262+65.5
32.2' LT
660.47

STA 262+49.2
27.2' LT
660.87

STA 262+54.2
27.2' LT
660.81

STA 262+60.6
27.2' LT
660.53

STA 262+49.2
23.7' LT
660.64

STA 262+45.9
22.3' LT
661.14

STA 262+54.2
23.7' LT
660.58

MATCH EXISTING
SIDEWALK (TYP.)

MATCH EXISTING
PAN CROSS SLOPE

STH 96

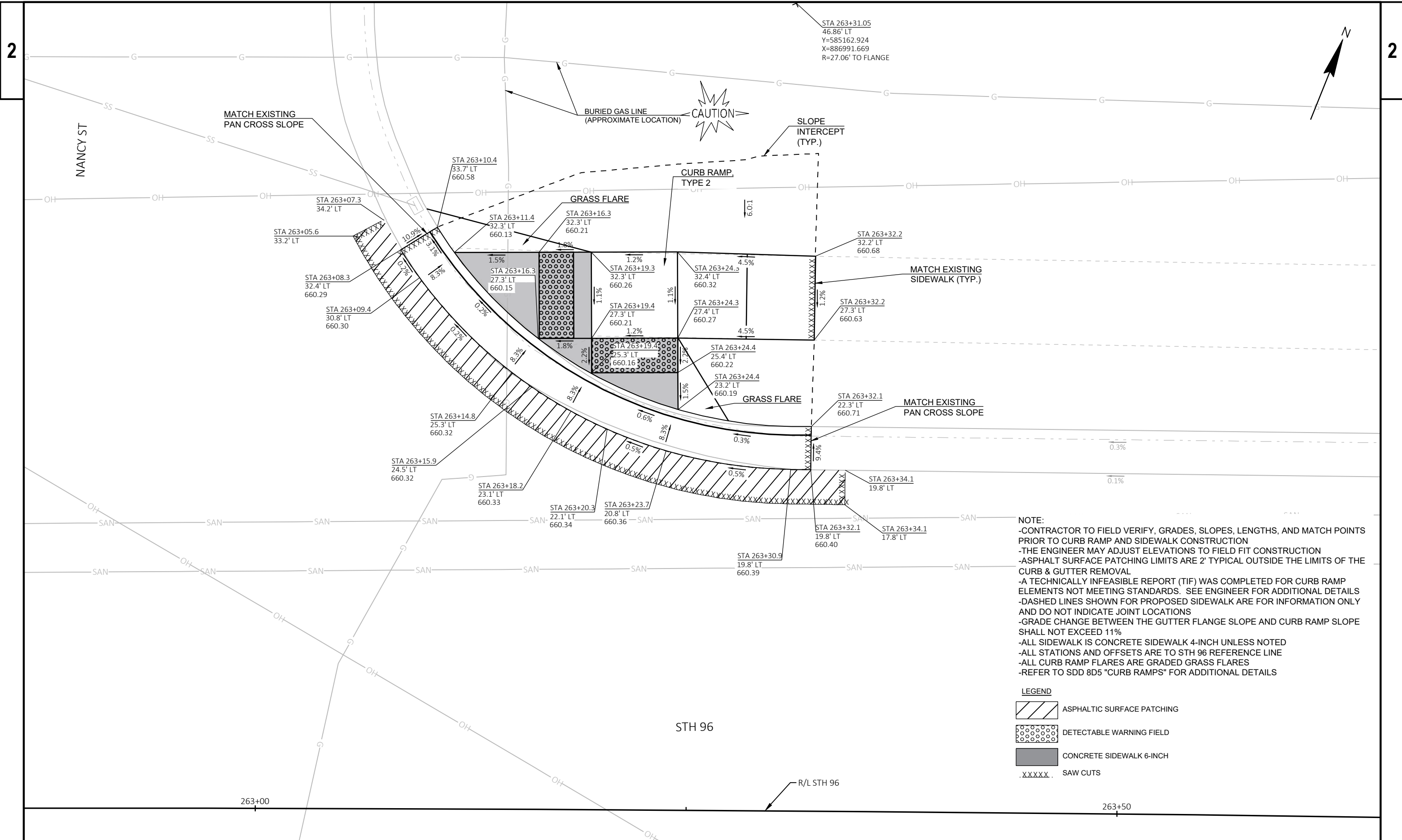
NANCY ST



NOTE:
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R/L STH 96





262+50

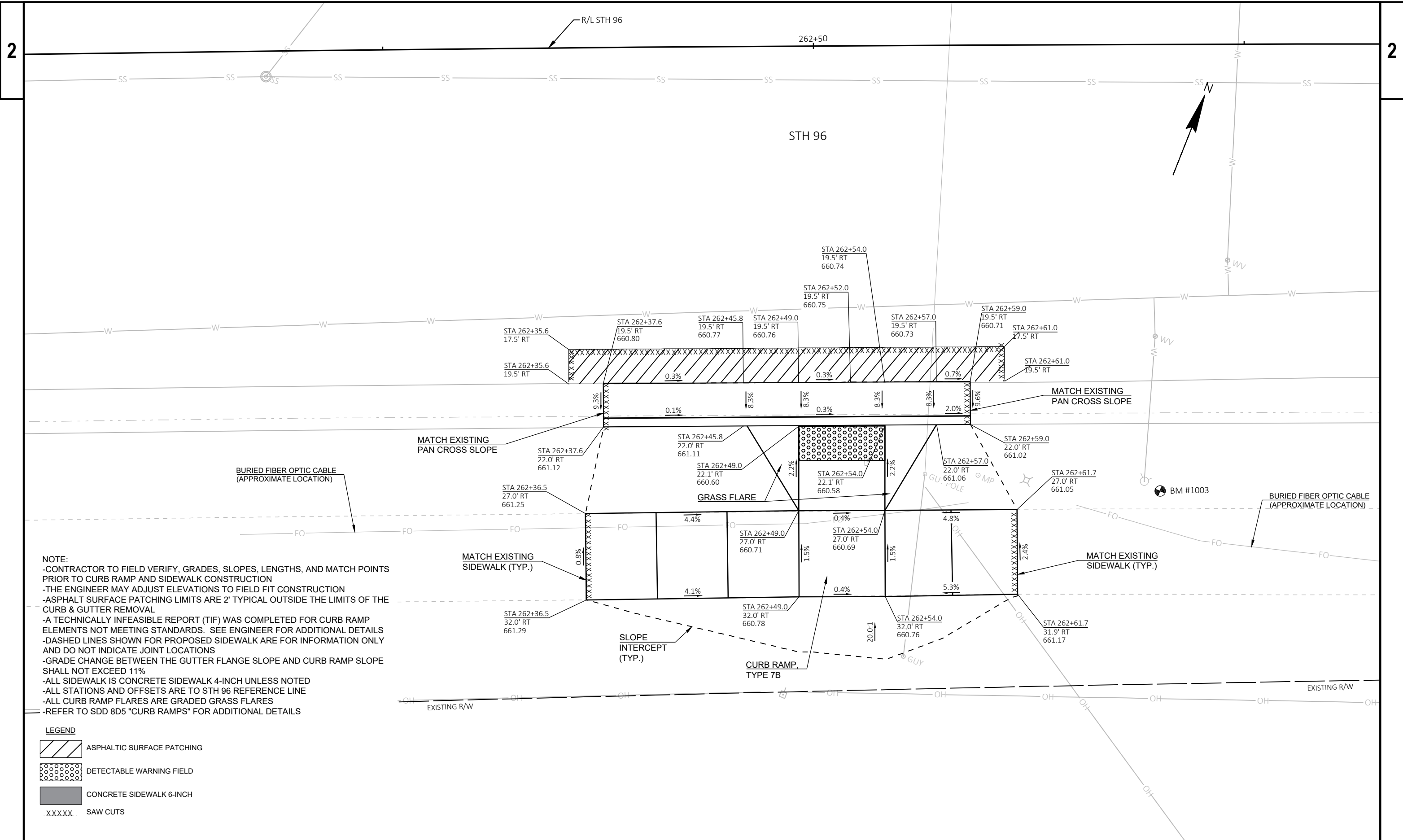


NOTE:





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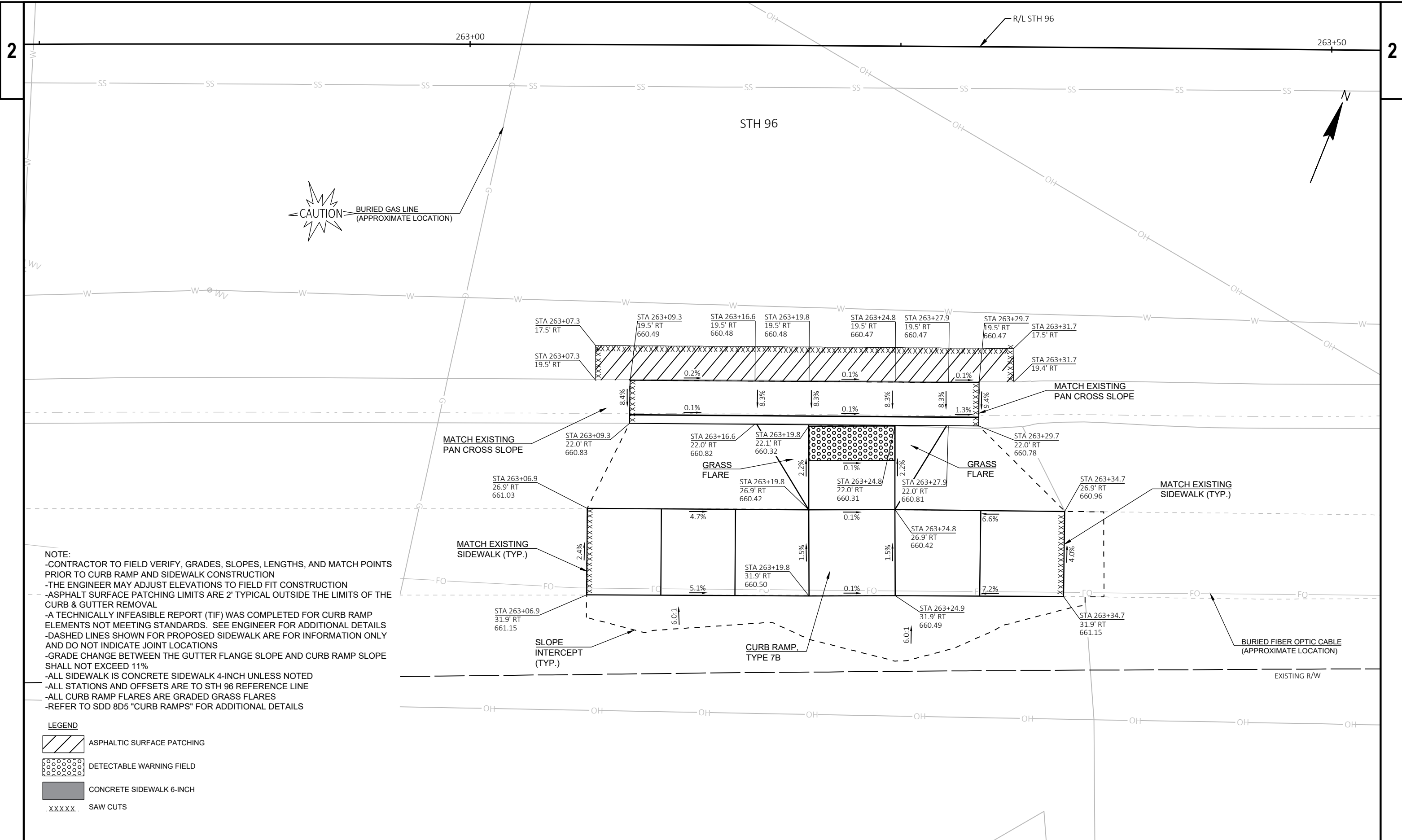
LEGEND

	ASPHALTIC SURFACE PATCHING
	DETECTABLE WARNING FIELD
	CONCRETE SIDEWALK 6-INCH
	SAW CUTS







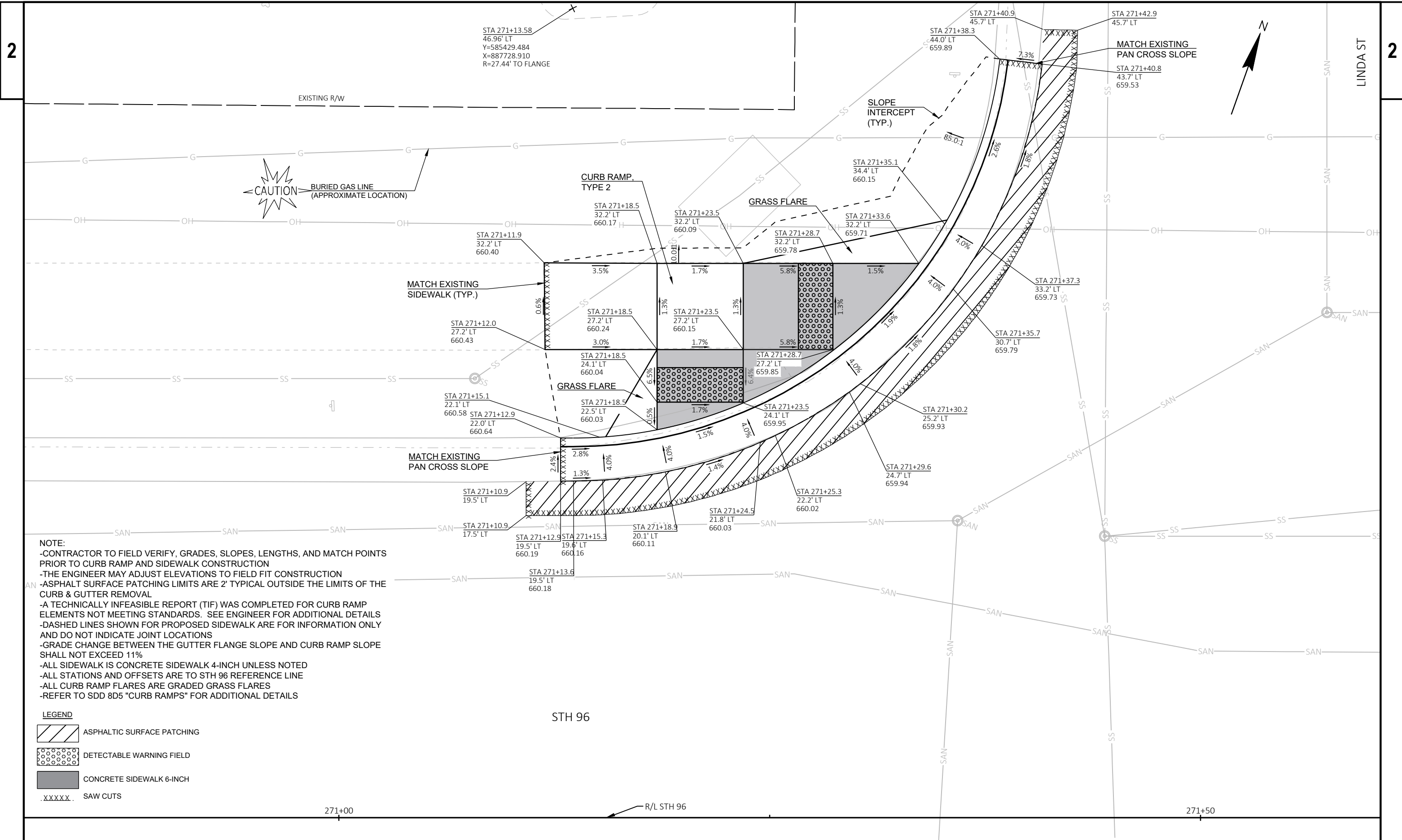
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- LEGEND**
-  ASPHALTIC SURFACE PATCHING
 -  DETECTABLE WARNING FIELD
 -  CONCRETE SIDEWALK 6-INCH
 -  .XXXXX. SAW CUTS



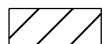


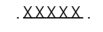
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 -ALL STATIONS AND OFFSETS ARE TO STH 96 REFERENCE LINE
 -ALL CURB RAMP FLARES ARE GRADED GRASS FLARES
 -REFER TO SDD 8D5 "CURB RAMPS" FOR ADDITIONAL DETAILS

- LEGEND**
-  ASPHALTIC SURFACE PATCHING
 -  DETECTABLE WARNING FIELD
 -  CONCRETE SIDEWALK 6-INCH
 -  .XXXXX. SAW CUTS

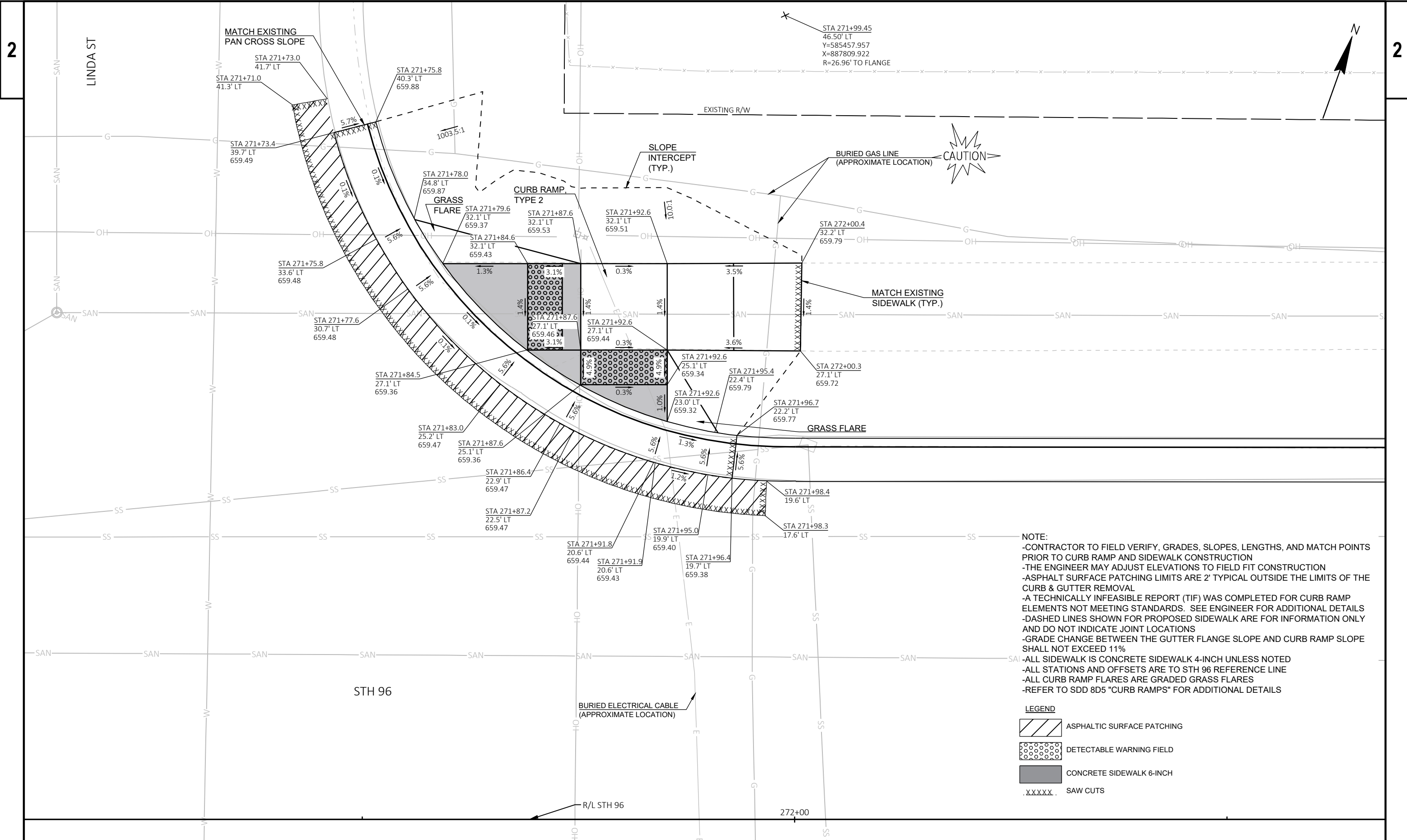


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 -  CONCRETE SIDEWALK 6-INCH
 -  SAW CUTS





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



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LEGEND

	ASPHALTIC SURFACE PATCHING
	DETECTABLE WARNING FIELD
	CONCRETE SIDEWALK 6-INCH
	SAW CUTS

LEGEND

-  ASPHALTIC SURFACE PATCHING
-  DETECTABLE WARNING FIELD
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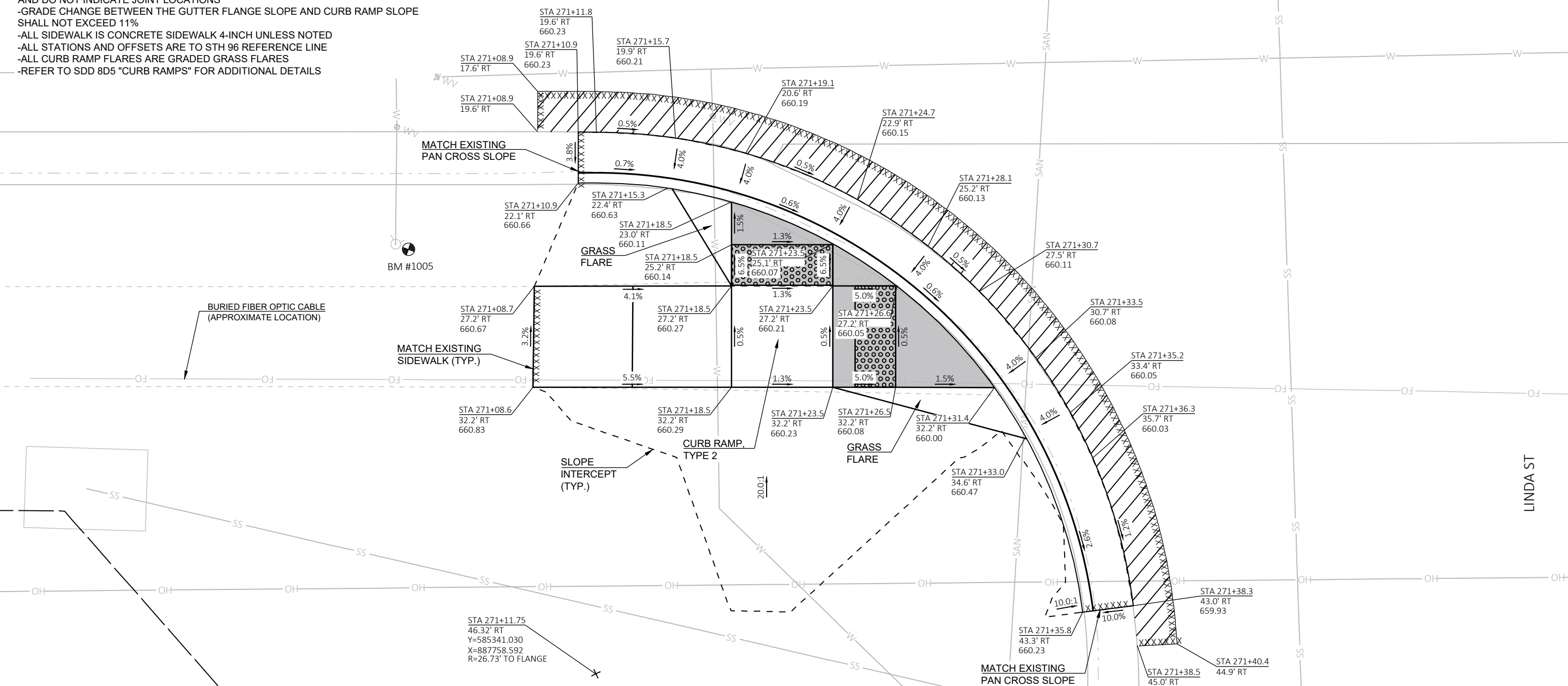
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STH 96



271+00

271+50



STA 271+11.8
19.6' RT
660.23

STA 271+10.9
19.6' RT
660.23

STA 271+15.7
19.9' RT
660.21

STA 271+19.1
20.6' RT
660.19

STA 271+24.7
22.9' RT
660.15

STA 271+28.1
25.2' RT
660.13

STA 271+30.7
27.5' RT
660.11

STA 271+33.5
30.7' RT
660.08

STA 271+35.2
33.4' RT
660.05

STA 271+36.3
35.7' RT
660.03

STA 271+38.3
43.0' RT
659.93

STA 271+40.4
44.9' RT

STA 271+38.5
45.0' RT

STA 271+35.8
43.3' RT
660.23

STA 271+33.0
34.6' RT
660.47

STA 271+26.5
32.2' RT
660.08

STA 271+31.4
32.2' RT
660.00

STA 271+23.5
32.2' RT
660.23

STA 271+18.5
32.2' RT
660.29

STA 271+26.6
27.2' RT
660.05

STA 271+23.5
27.2' RT
660.21

STA 271+18.5
25.2' RT
660.14

STA 271+18.5
23.0' RT
660.11

STA 271+15.3
22.4' RT
660.63

STA 271+10.9
22.1' RT
660.66

STA 271+10.9
19.6' RT
660.23

STA 271+08.9
19.6' RT

STA 271+08.9
17.6' RT


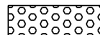


STA 271+08.7
27.2' RT
660.67

STA 271+08.6
32.2' RT
660.83

STA 271+11.75
46.32' RT
Y=585341.030
X=887758.592
R=26.73' TO FLANGE

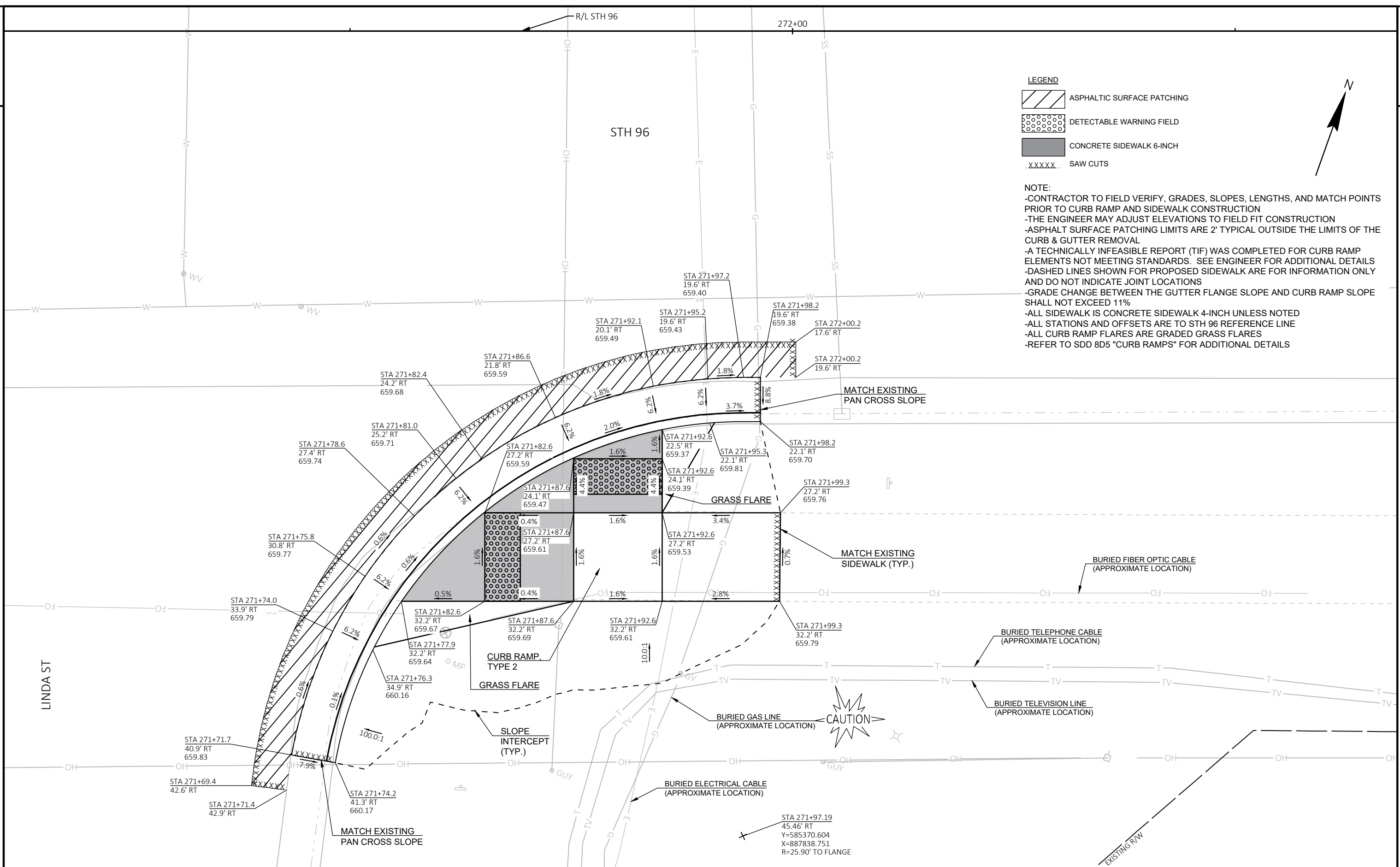
LINDA ST

LEGEND

-  ASPHALTIC SURFACE PATCHING
-  DETECTABLE WARNING FIELD
-  CONCRETE SIDEWALK 6-INCH
-  .XXXXX. SAW CUTS

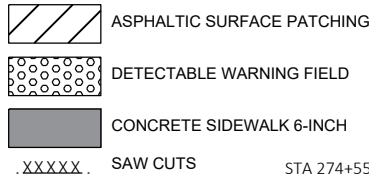
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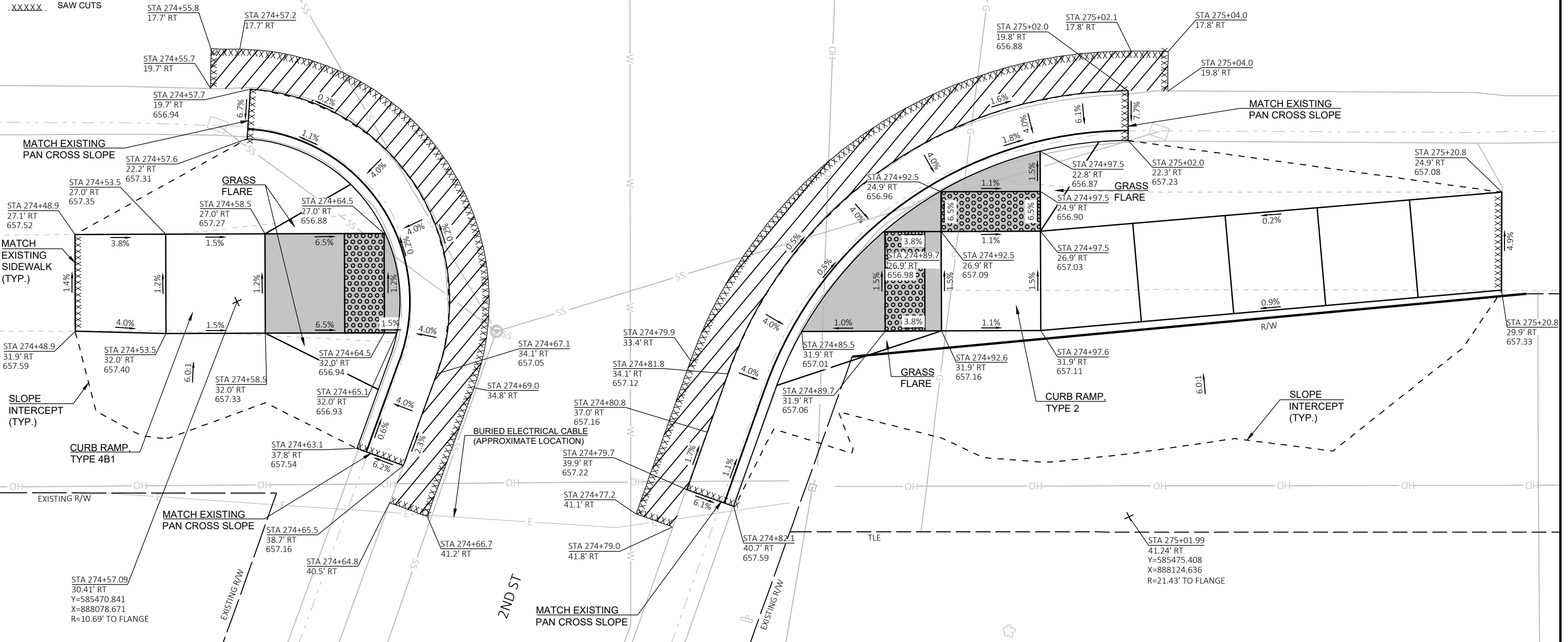
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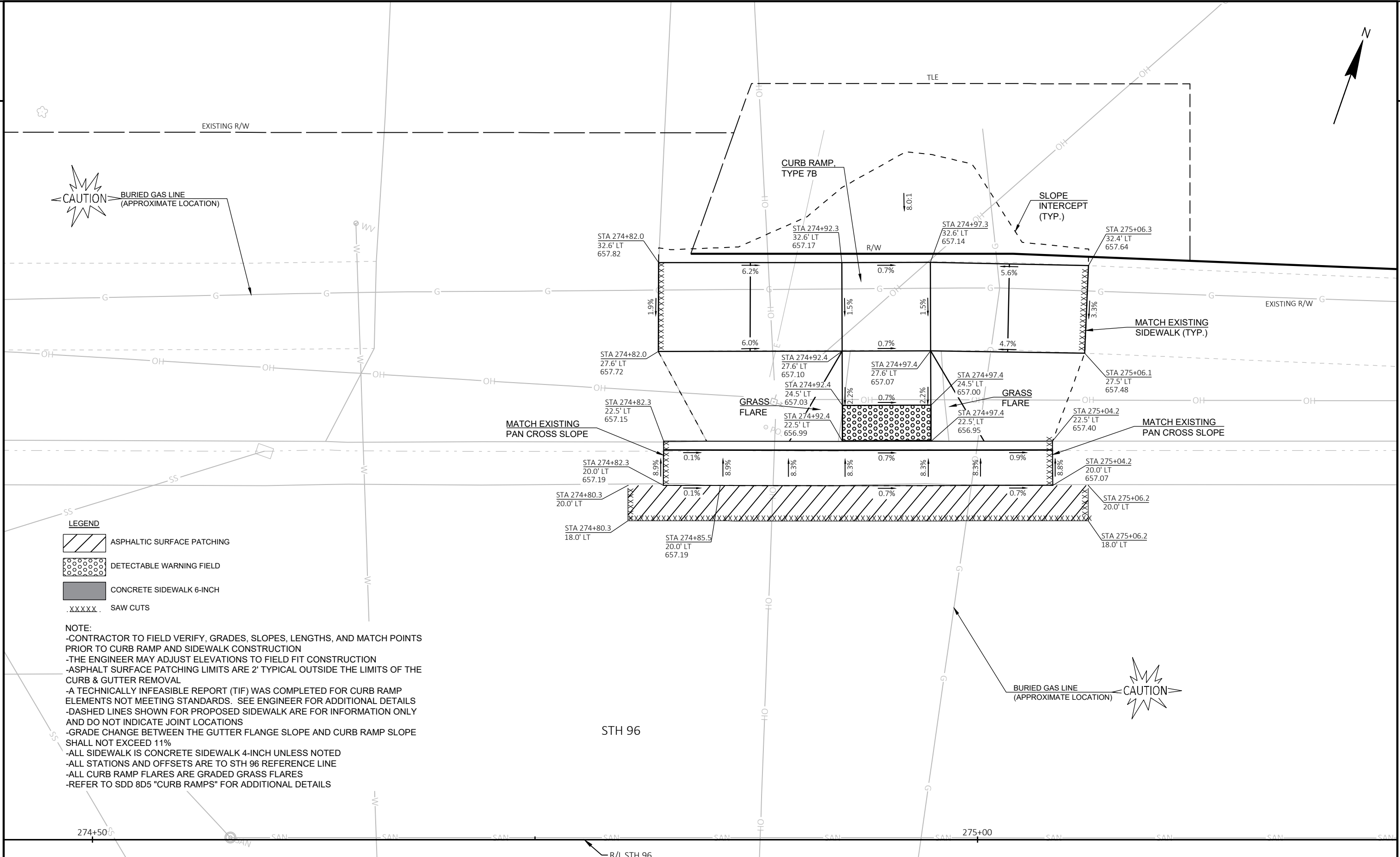


PROJECT NO: 4075-35-72	HWY: STH 96	COUNTY: BROWN	PLAN DETAILS - CURB RAMPS	SHEET	E
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


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LEGEND

 ASPHALTIC SURFACE PATCHING
 DETECTABLE WARNING FIELD
 CONCRETE SIDEWALK 6-INCH
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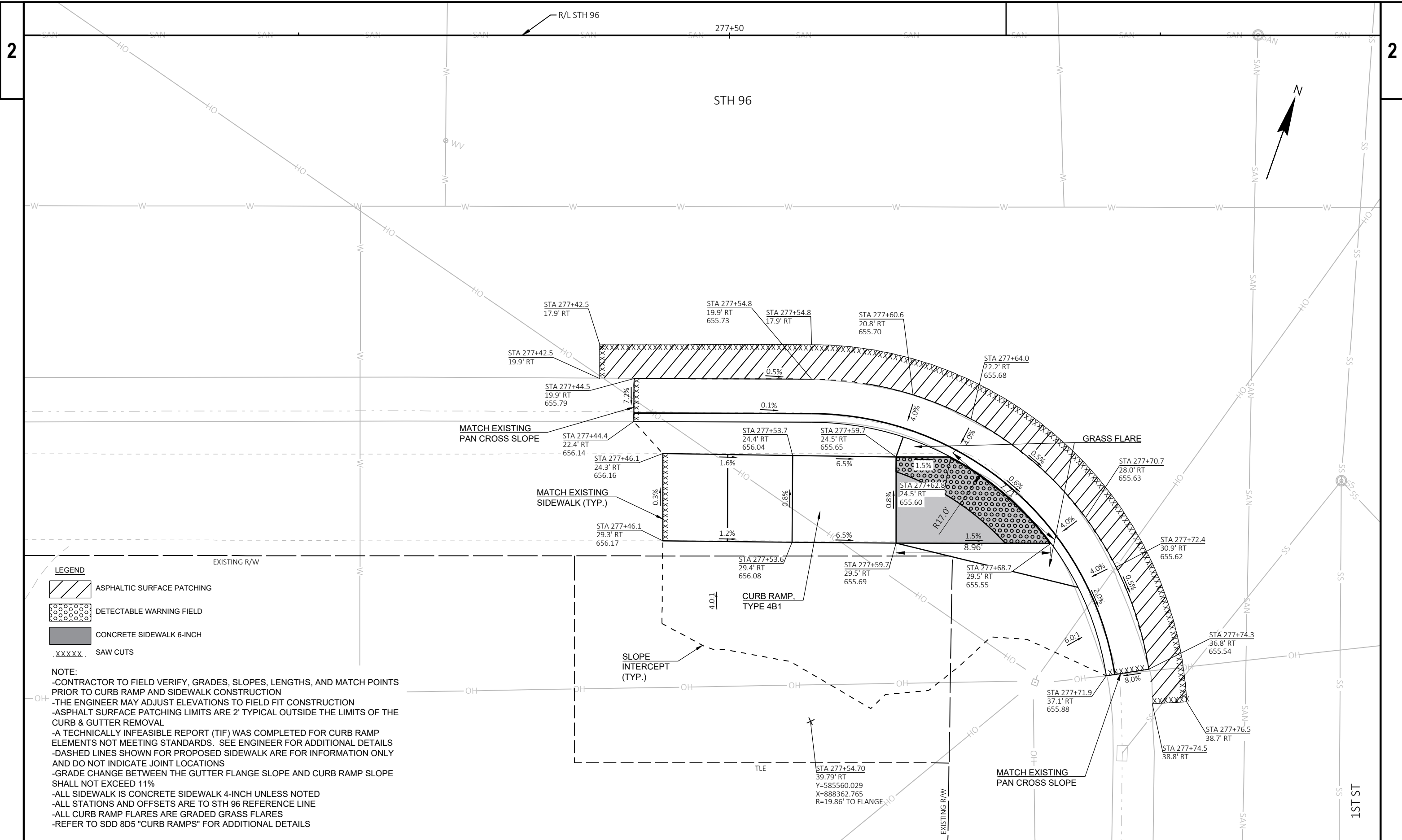
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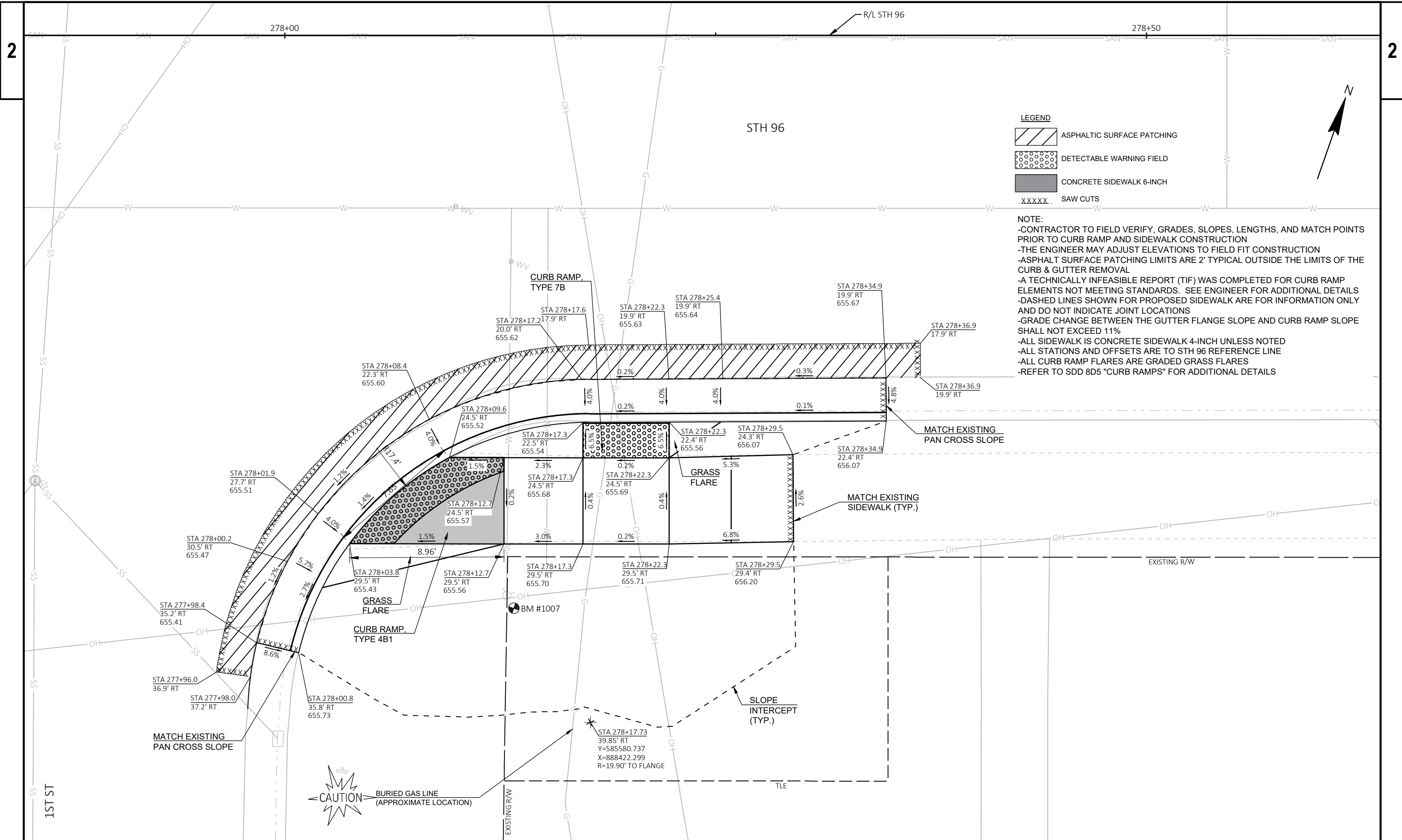
PROJECT NO: 4075-35-72	HWY: STH 96	COUNTY: BROWN	PLAN DETAILS - CURB RAMPS
SHEET			E







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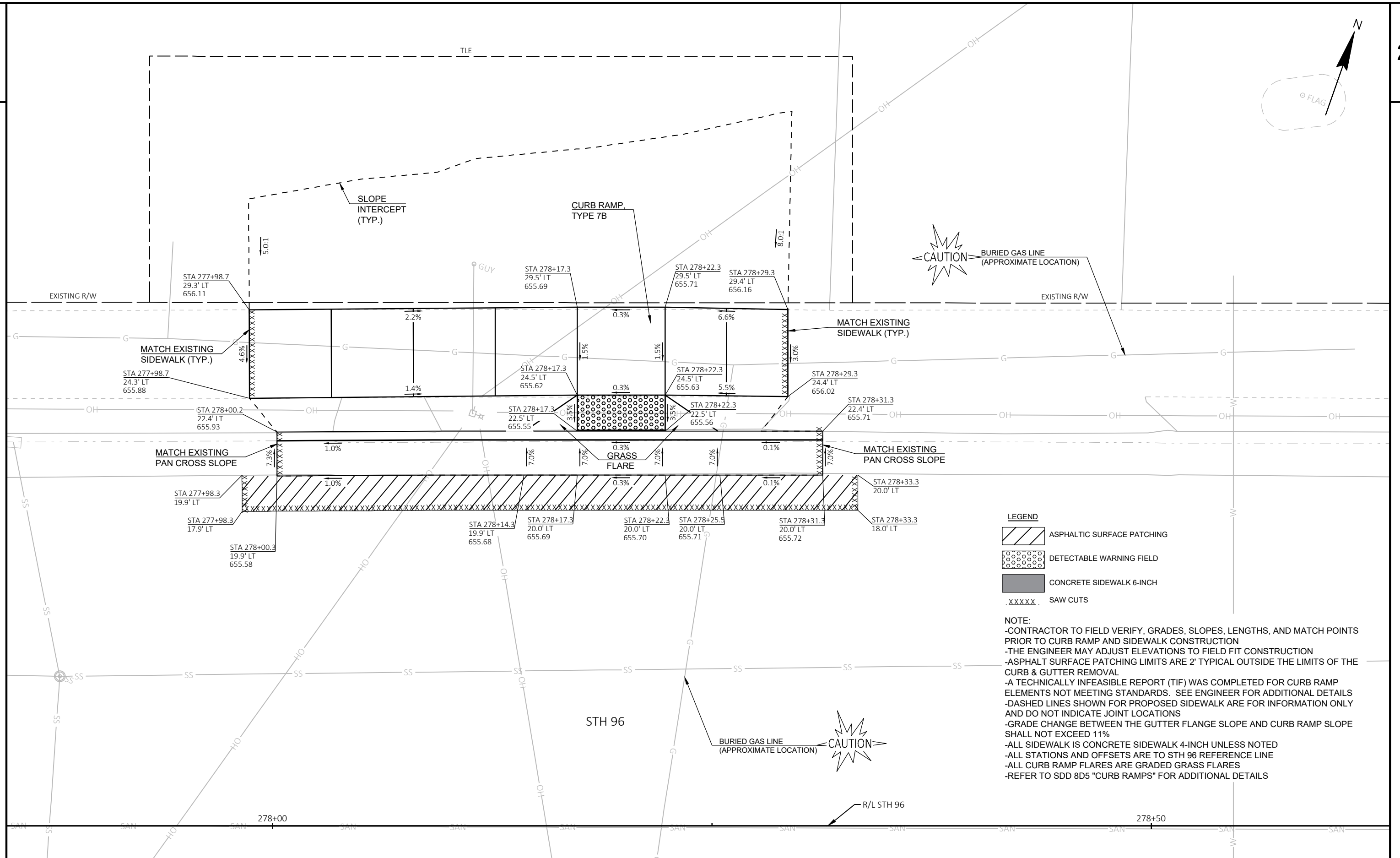


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



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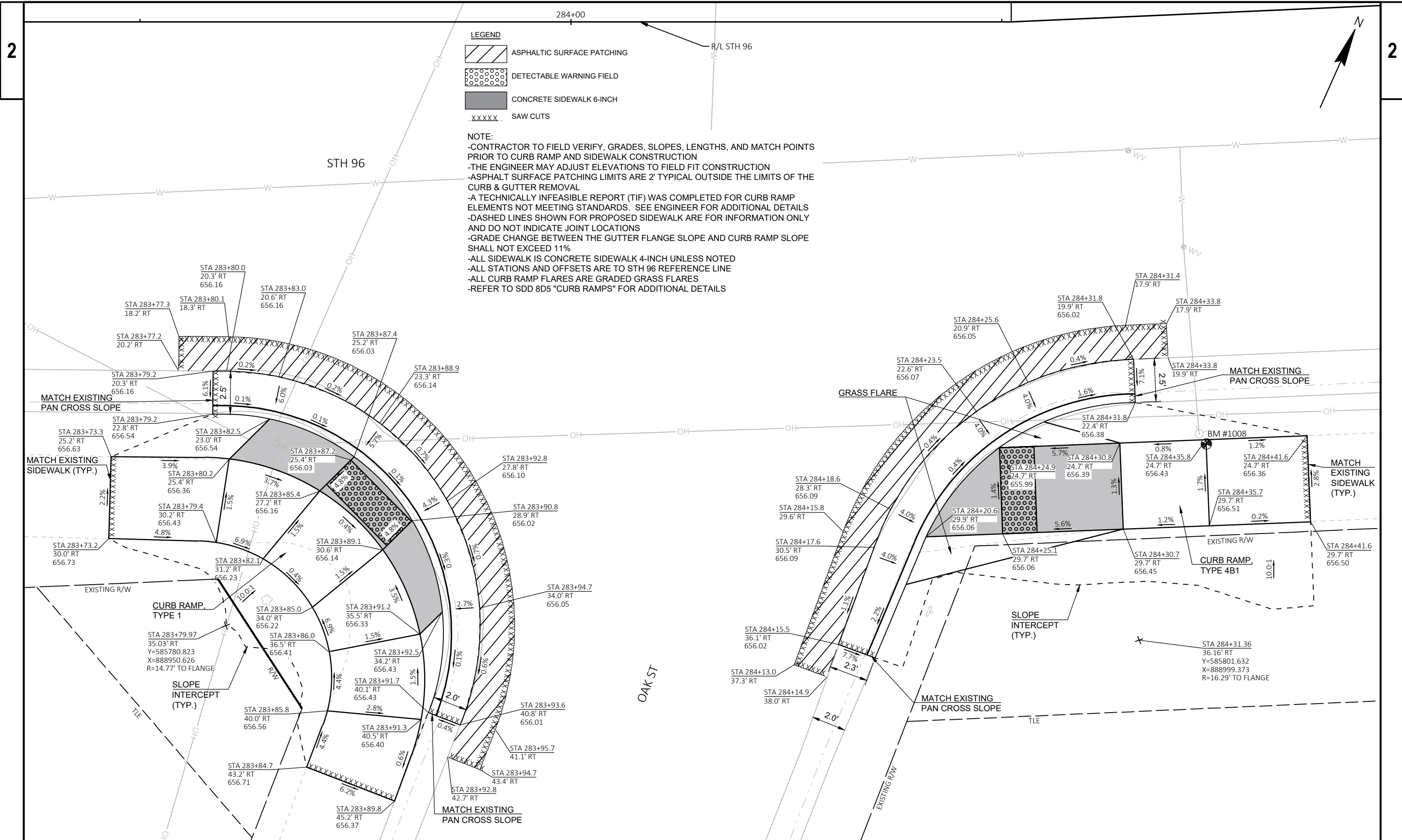


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PROJECT NO: 4075-35-72

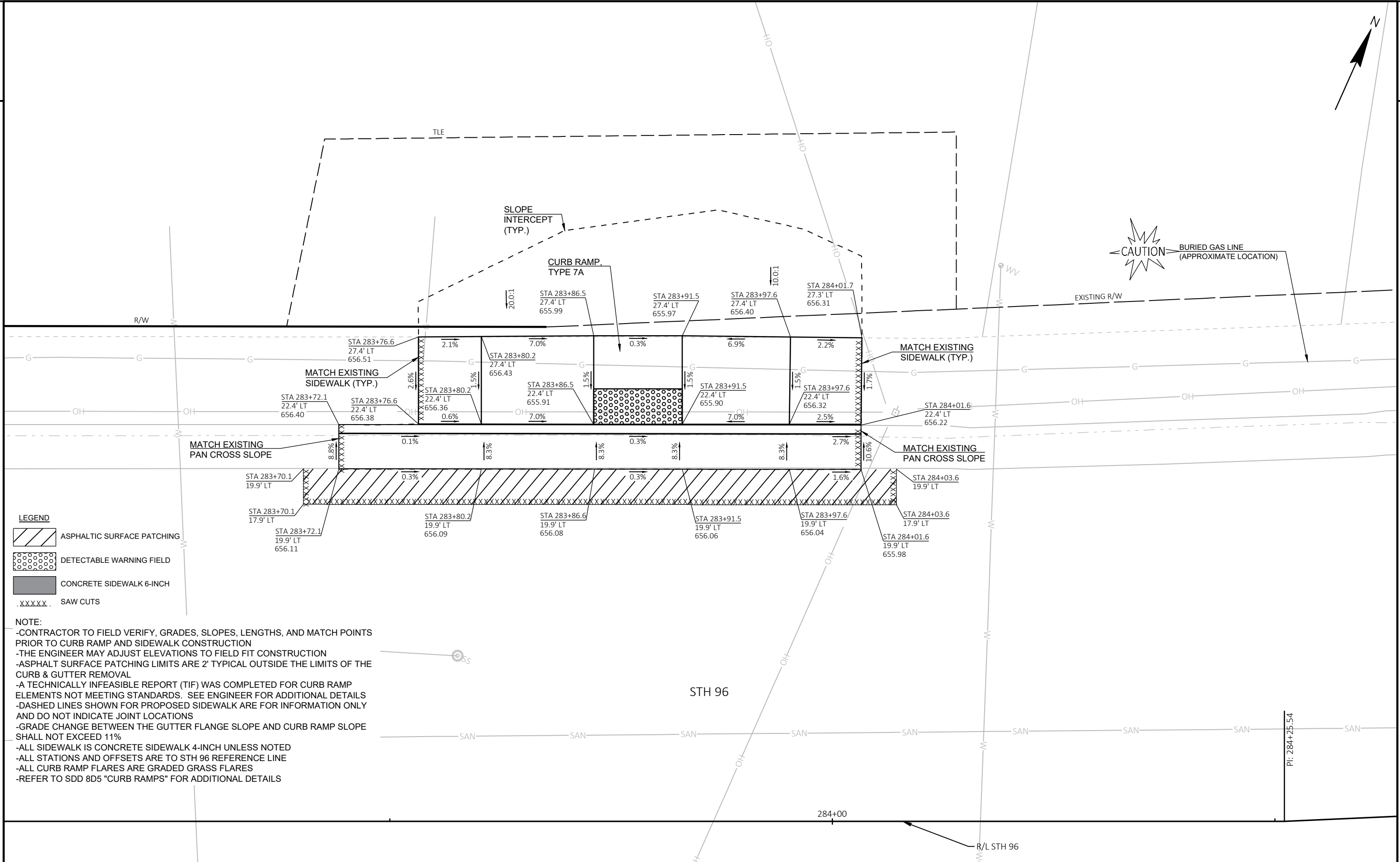
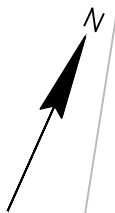
HWY: STH 96

COUNTY: BROWN

PLAN DETAILS - CURB RAMPS

SHEET

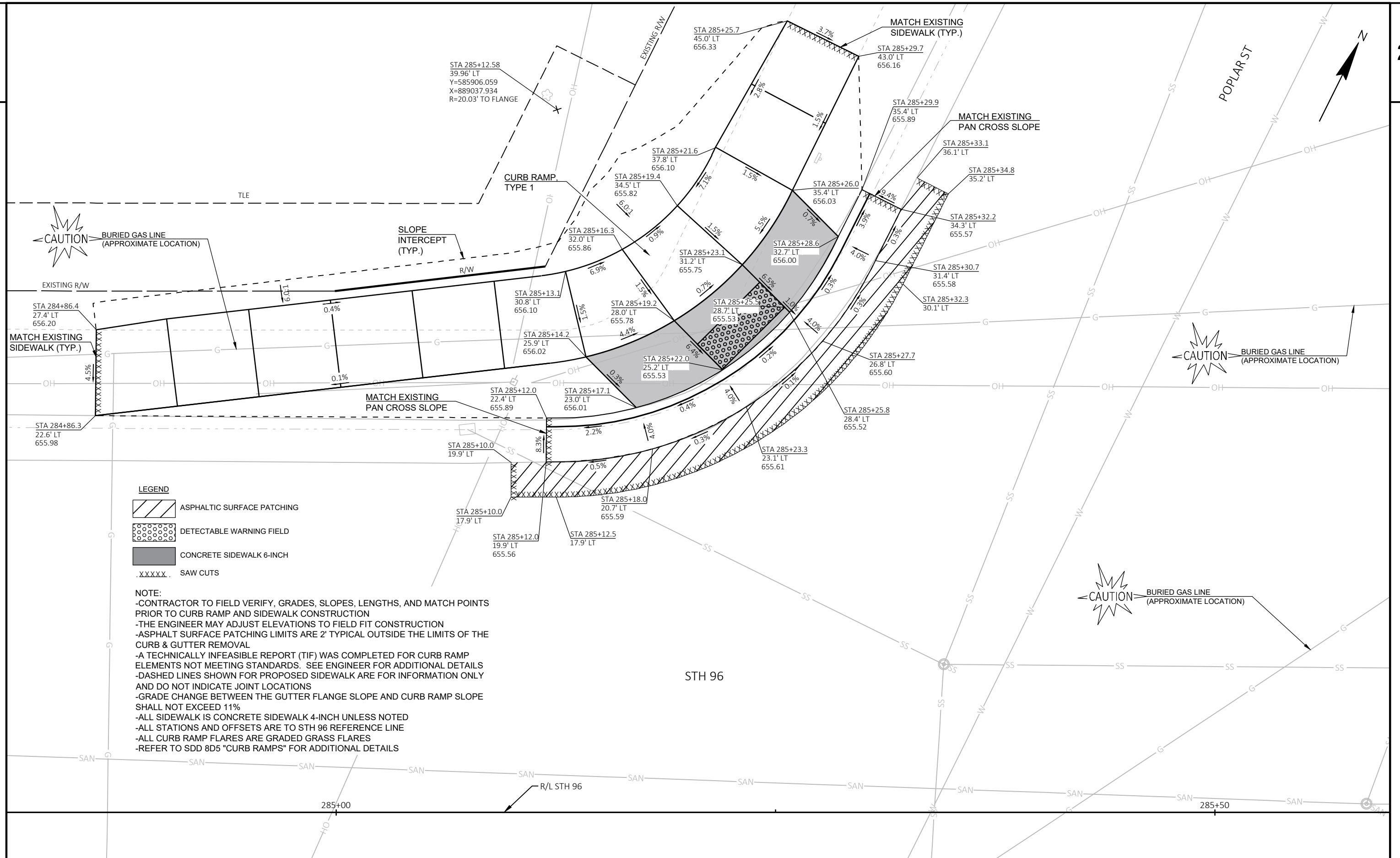
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- LEGEND**
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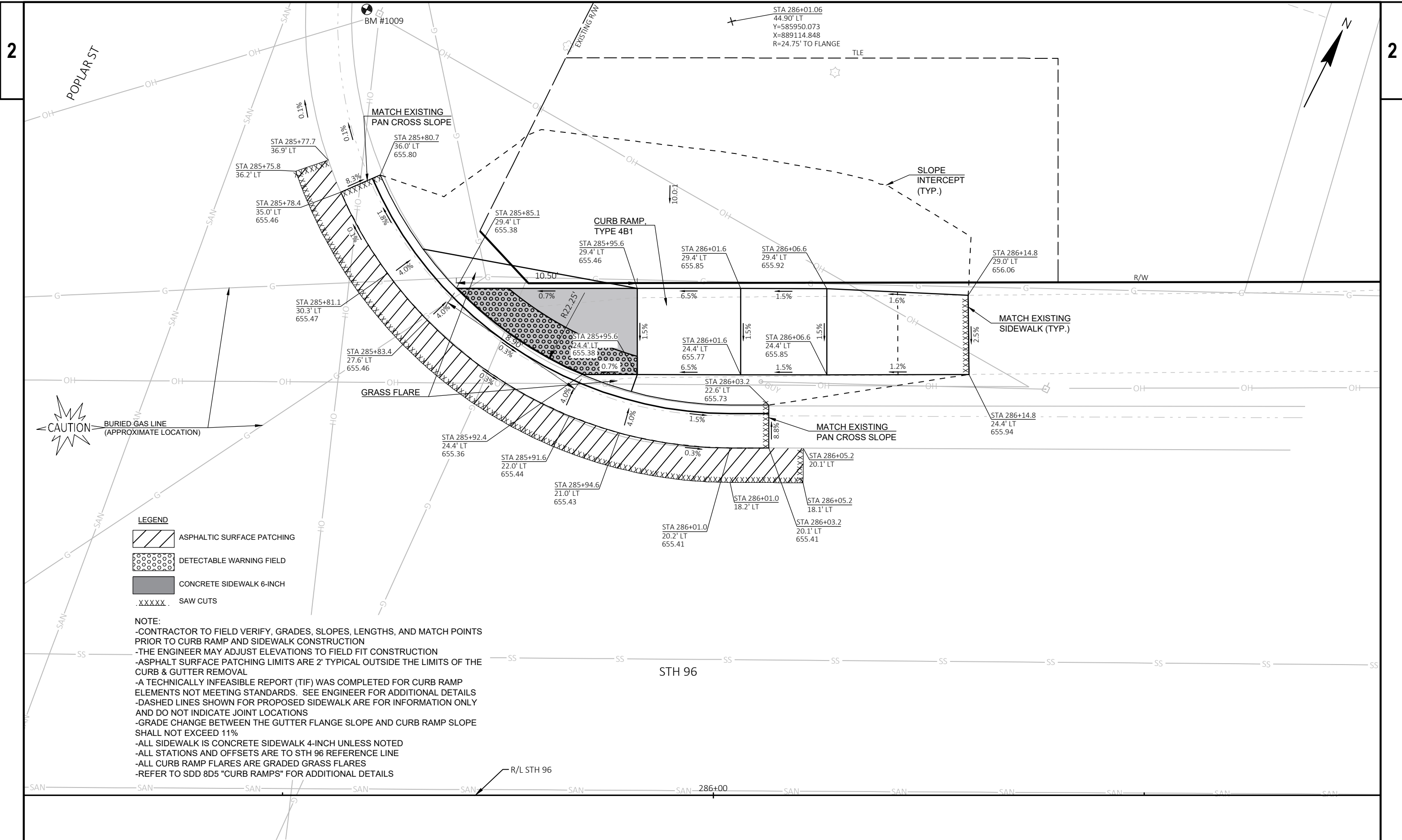
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



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- GRADE CHANGE BETWEEN THE GUTTER FLANGE SLOPE AND CURB RAMP SLOPE SHALL NOT EXCEED 11%
- ALL SIDEWALK IS CONCRETE SIDEWALK 4-INCH UNLESS NOTED
- ALL STATIONS AND OFFSETS ARE TO STH 96 REFERENCE LINE
- ALL CURB RAMP FLARES ARE GRADED GRASS FLARES
- REFER TO SDD 8D5 "CURB RAMPS" FOR ADDITIONAL DETAILS

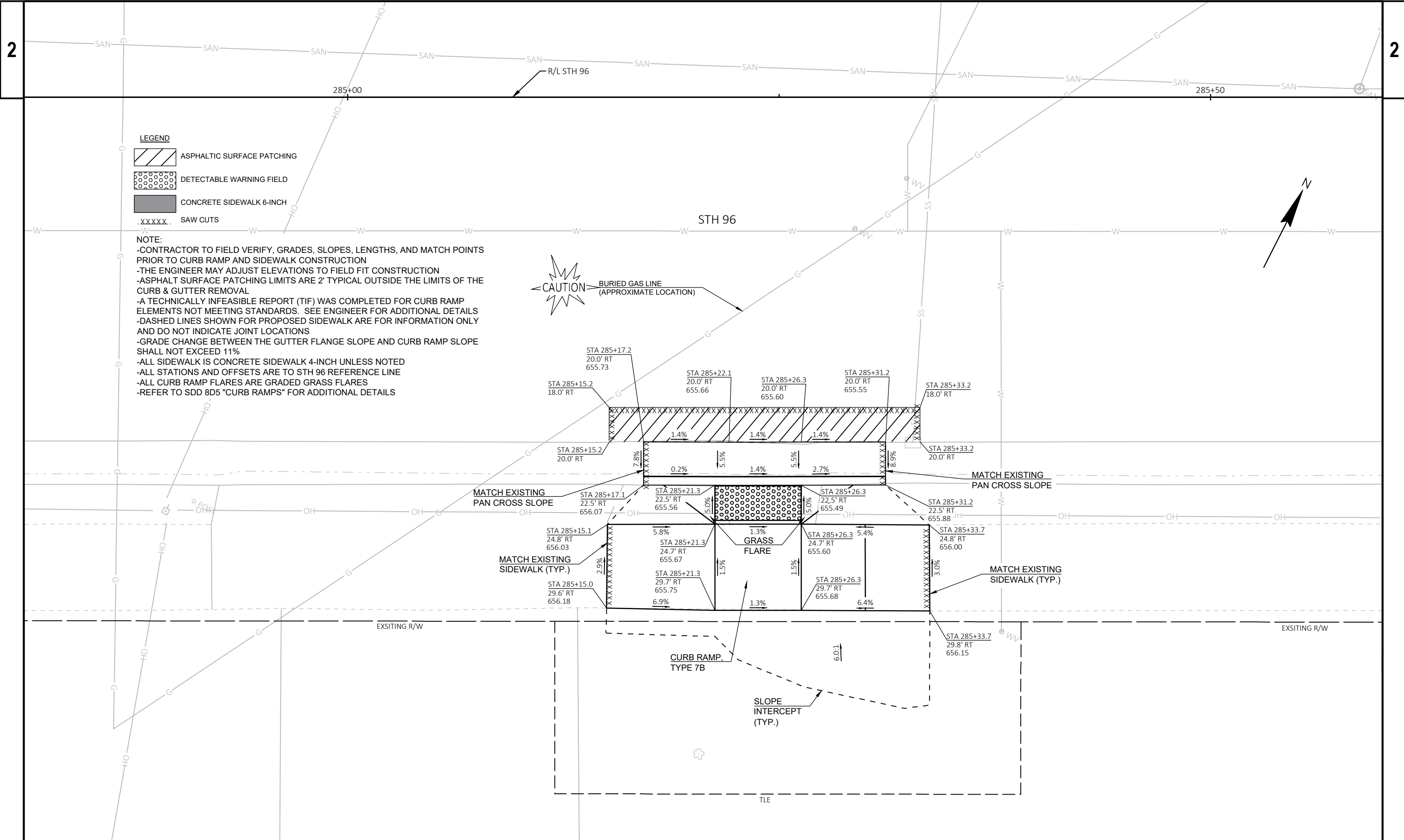


CAUTION BURIED GAS LINE (APPROXIMATE LOCATION)





- LEGEND**
-  ASPHALTIC SURFACE PATCHING
 -  DETECTABLE WARNING FIELD
 -  CONCRETE SIDEWALK 6-INCH
 -  .XXXXX. SAW CUTS

NOTE:

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LEGEND

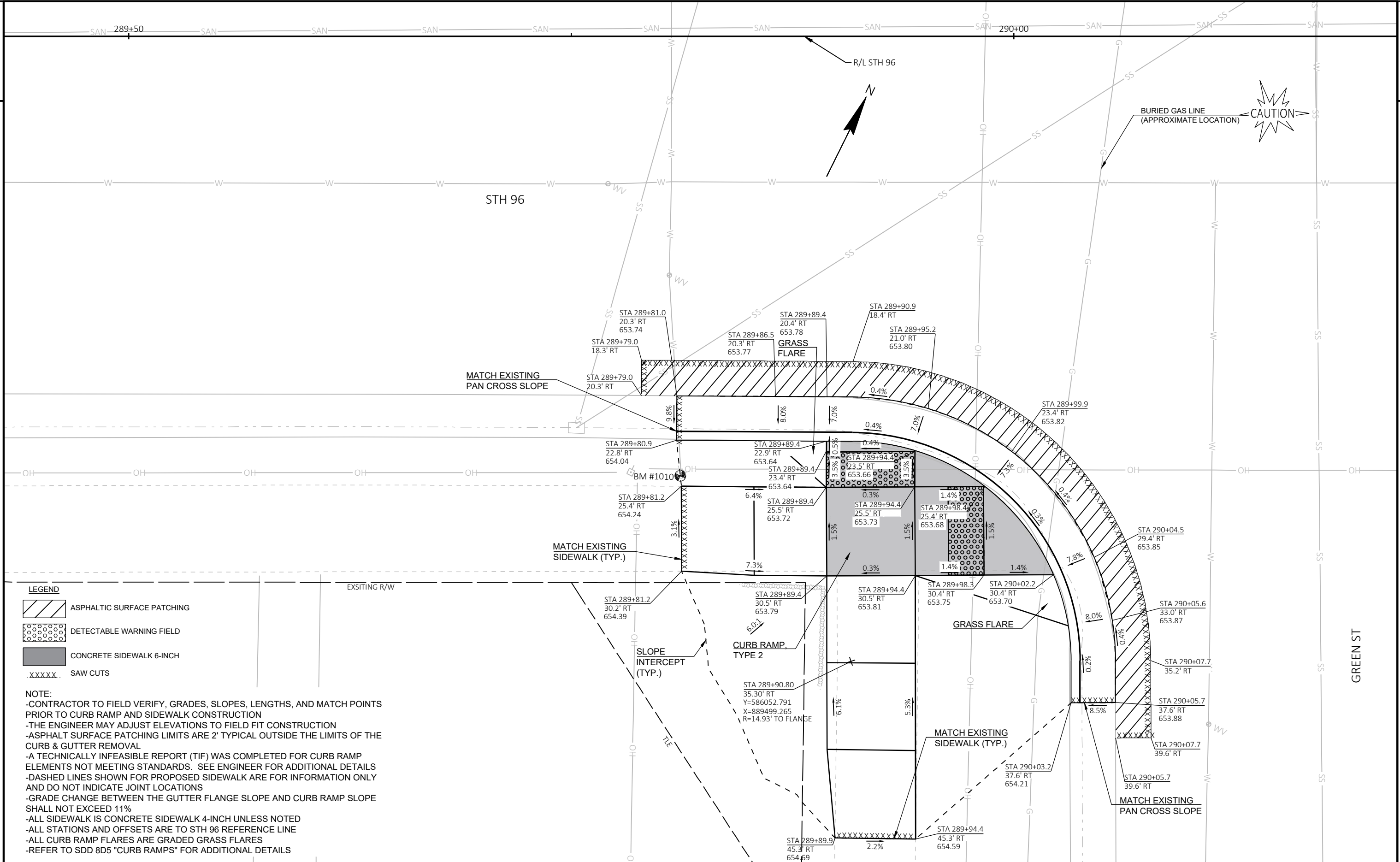
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-  DETECTABLE WARNING FIELD
-  CONCRETE SIDEWALK 6-INCH
-  .XXXXX. SAW CUTS





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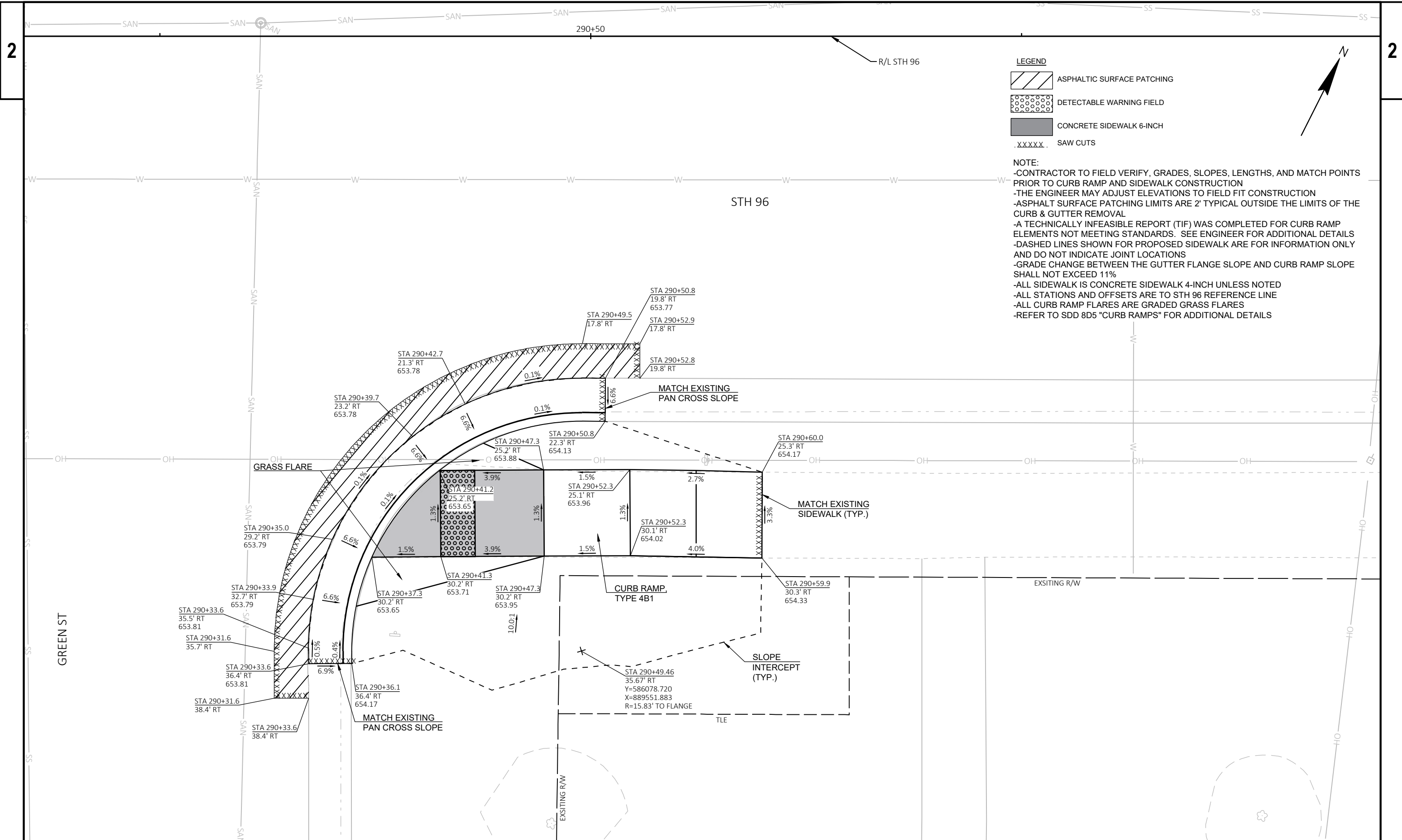








- LEGEND**
-  ASPHALTIC SURFACE PATCHING
 -  DETECTABLE WARNING FIELD
 -  CONCRETE SIDEWALK 6-INCH
 -  .XXXX. SAW CUTS

NOTE:

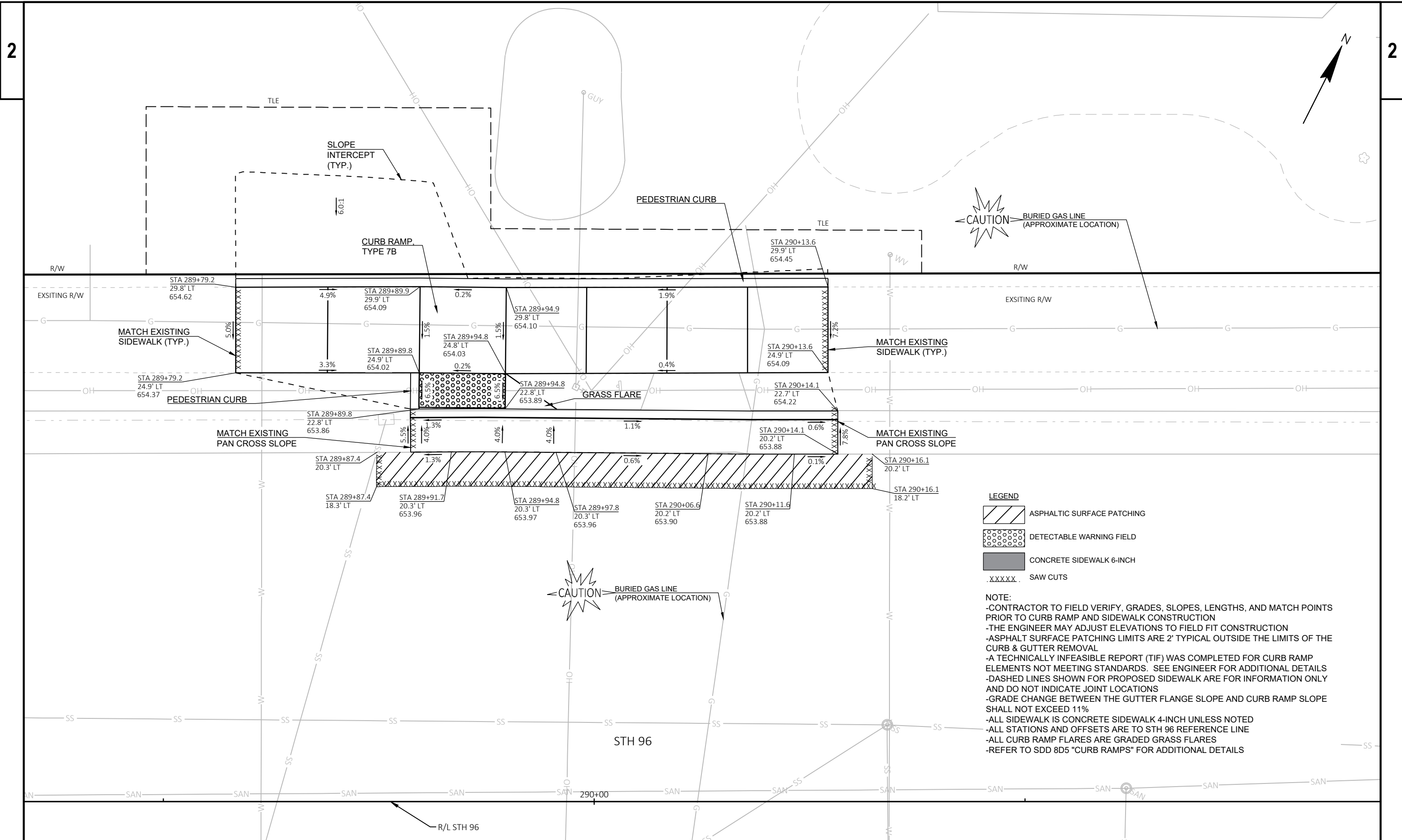
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


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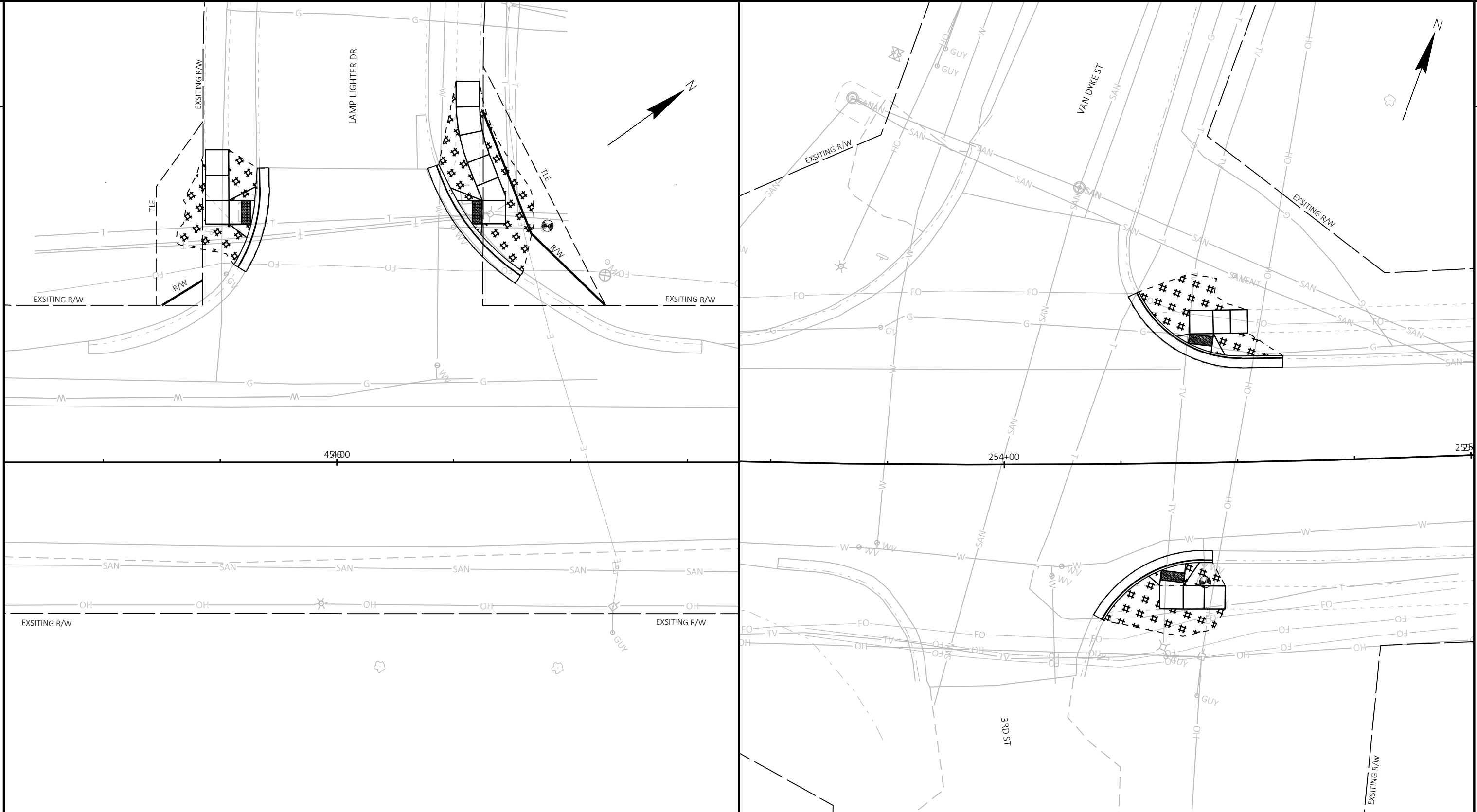


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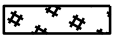


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-  DETECTABLE WARNING FIELD
-  CONCRETE SIDEWALK 6-INCH
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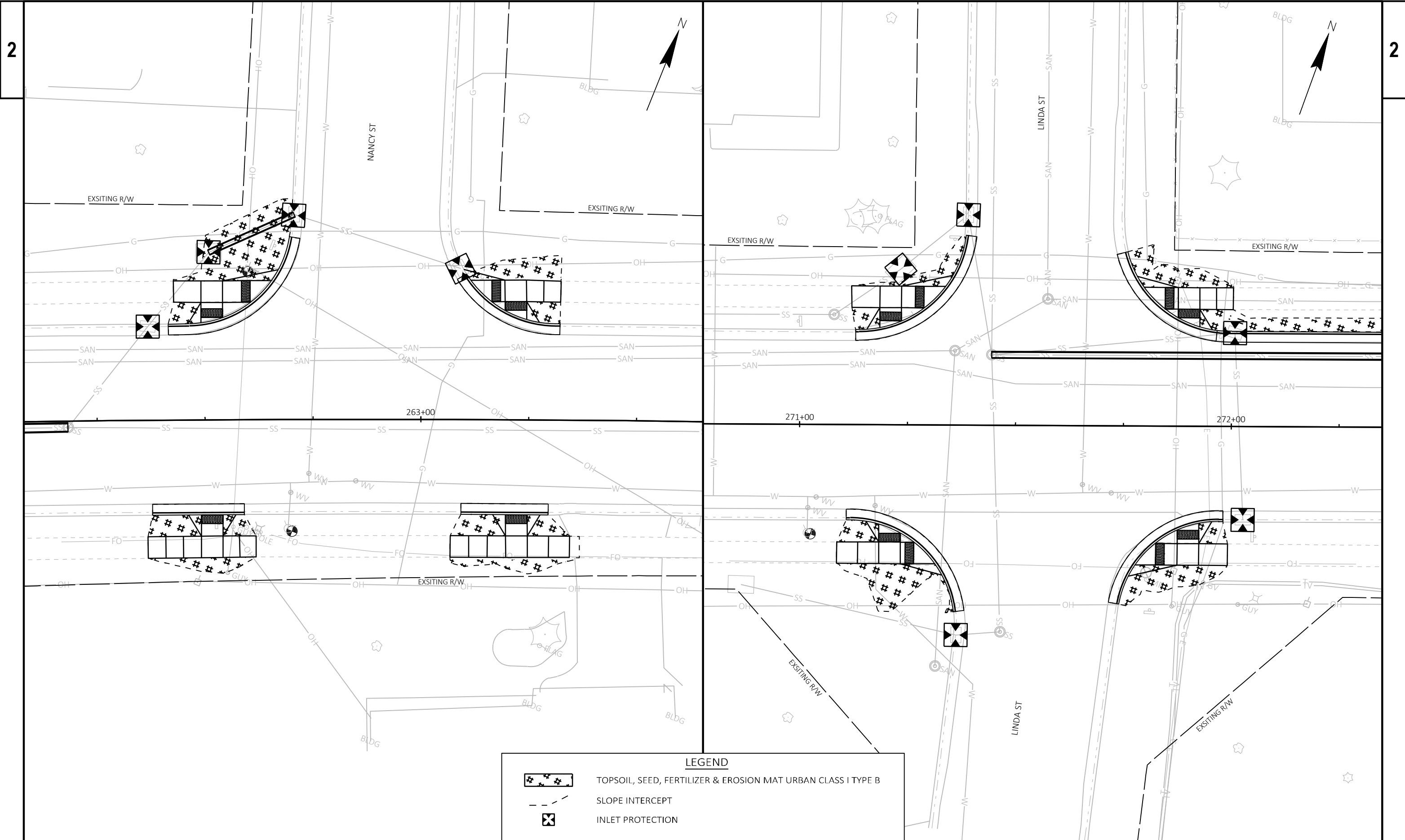
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LEGEND

	TOPSOIL, SEED, FERTILIZER & EROSION MAT URBAN CLASS I TYPE B
	SLOPE INTERCEPT
	INLET PROTECTION



PROJECT NO: 4075-35-71/72

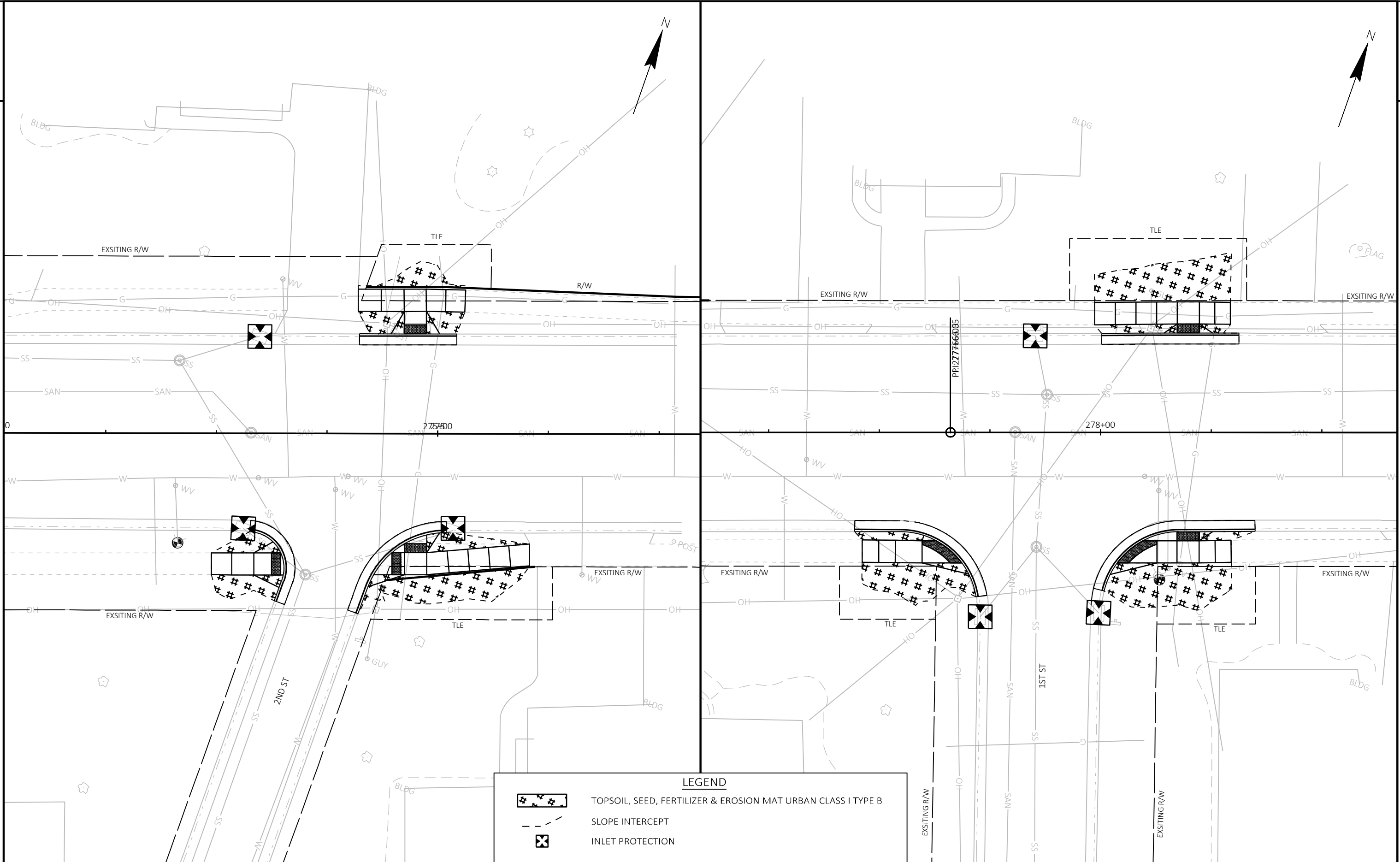
HWY: STH 96

COUNTY: BROWN & OUTAGAMIE

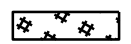


EROSION CONTROL

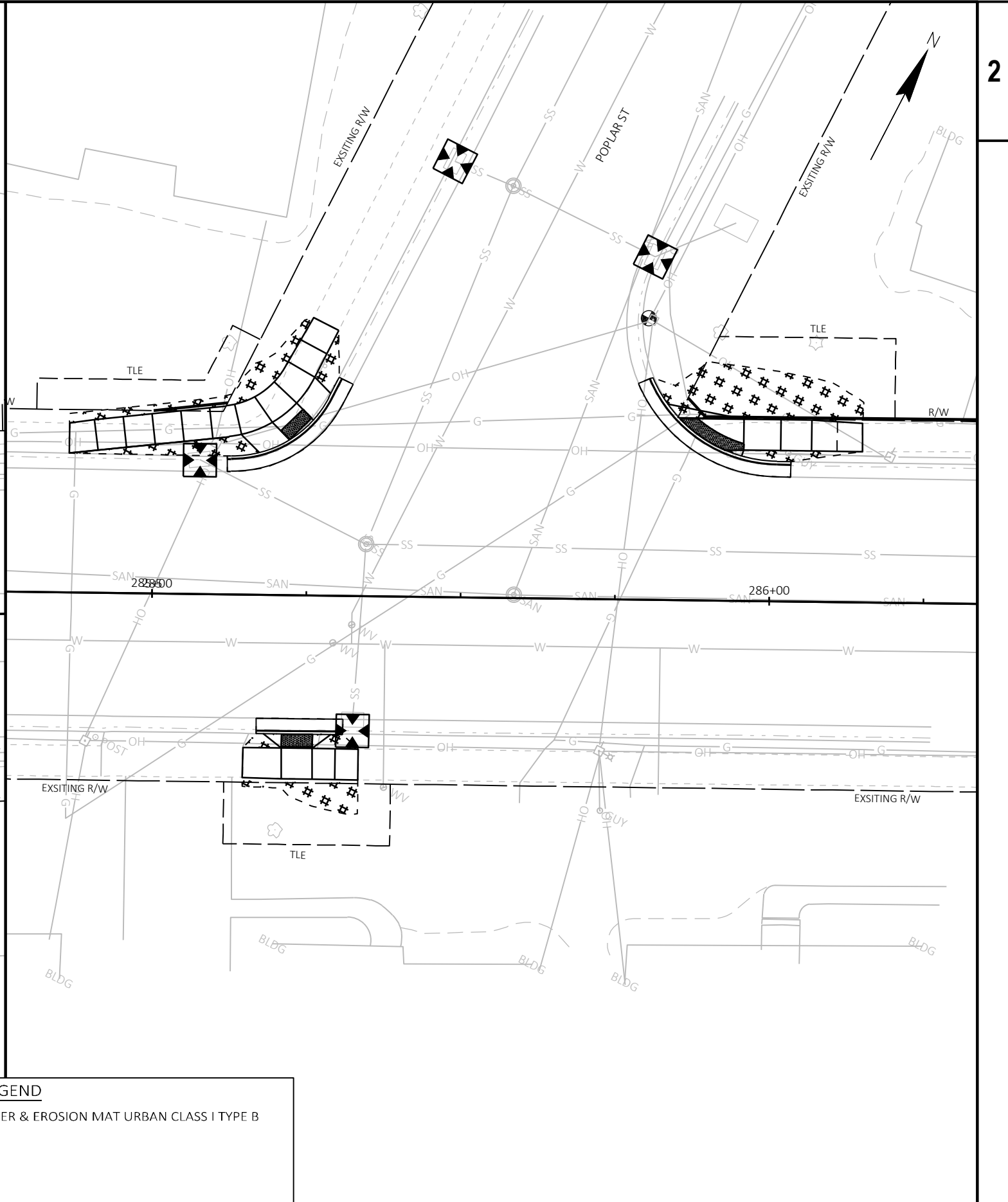
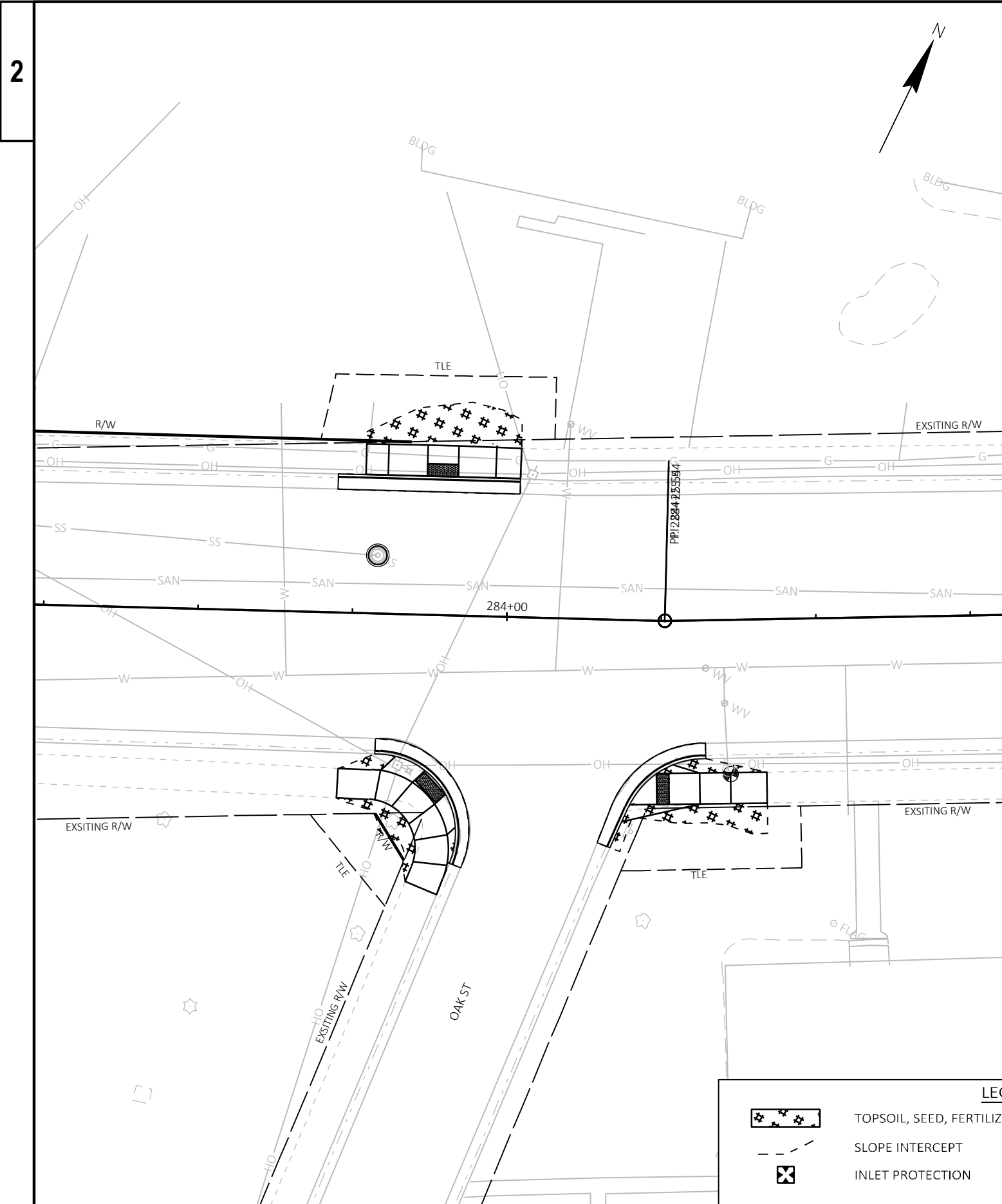
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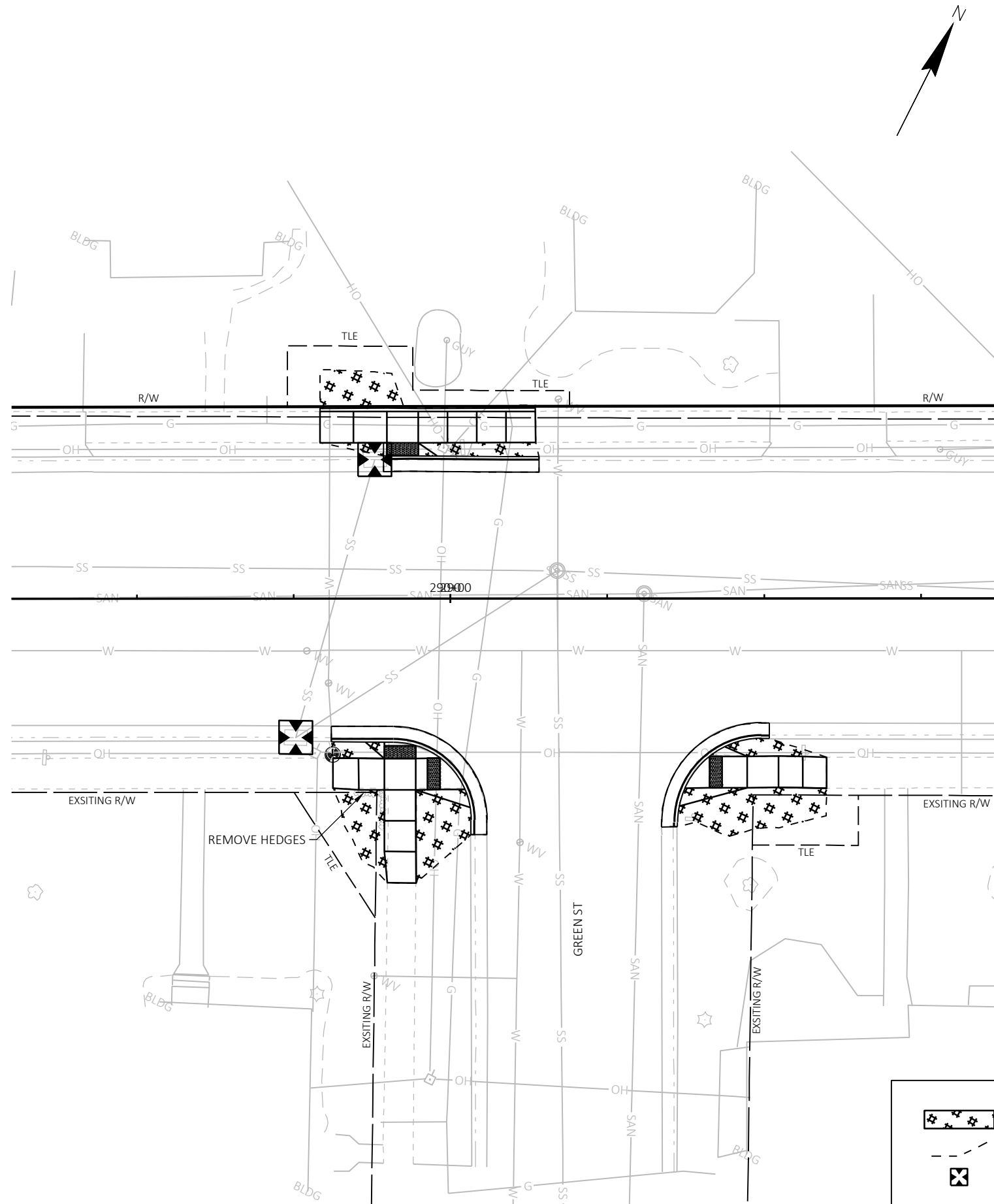
LEGEND

	TOPSOIL, SEED, FERTILIZER & EROSION MAT URBAN CLASS I TYPE B
	SLOPE INTERCEPT
	INLET PROTECTION



LEGEND

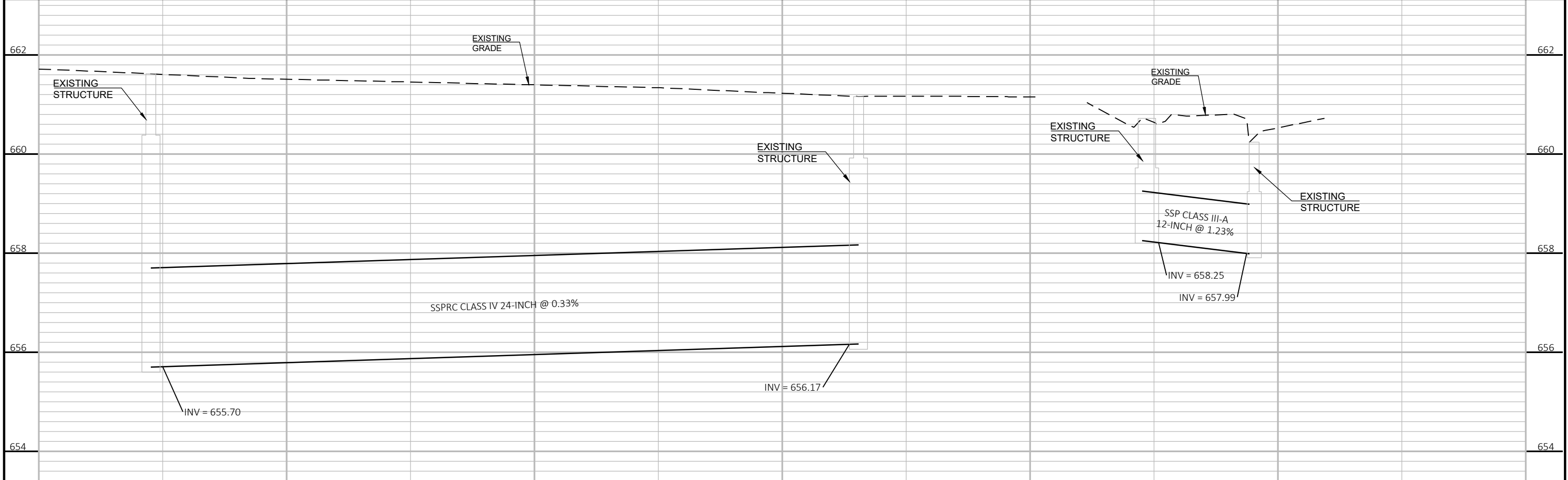
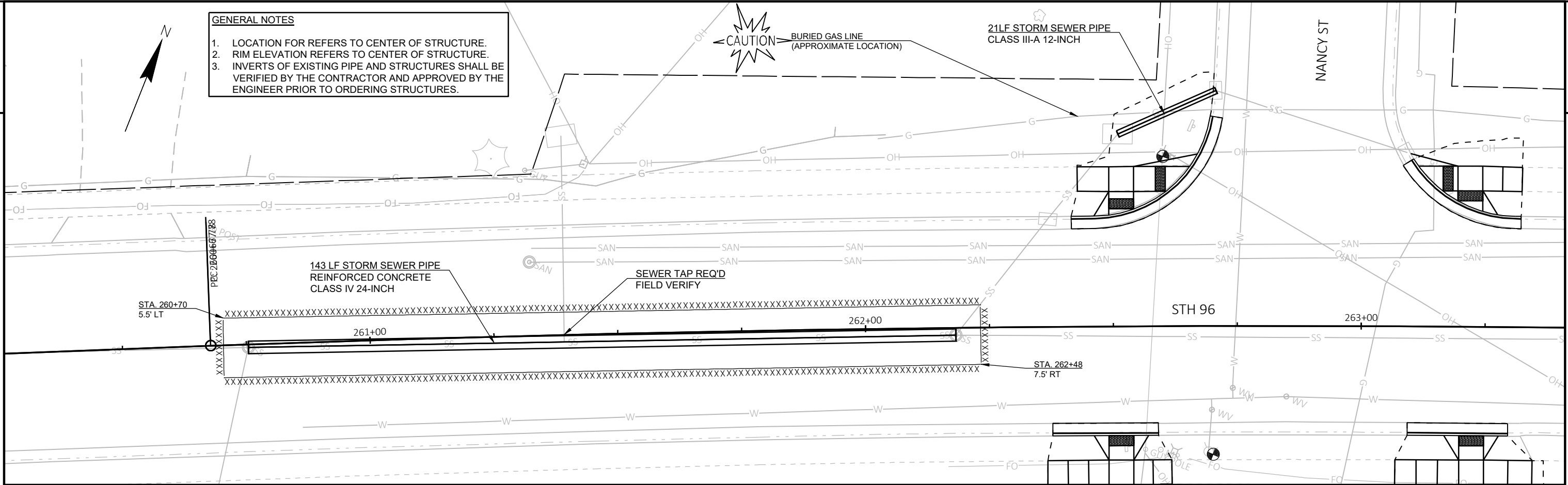
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	SLOPE INTERCEPT
	INLET PROTECTION



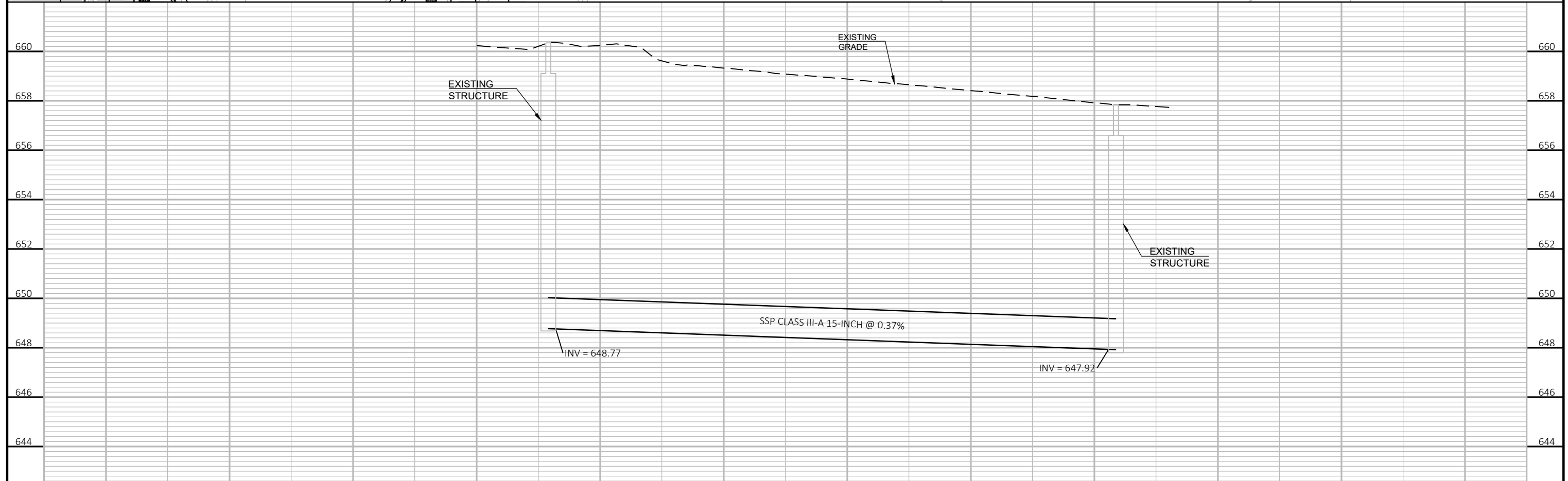
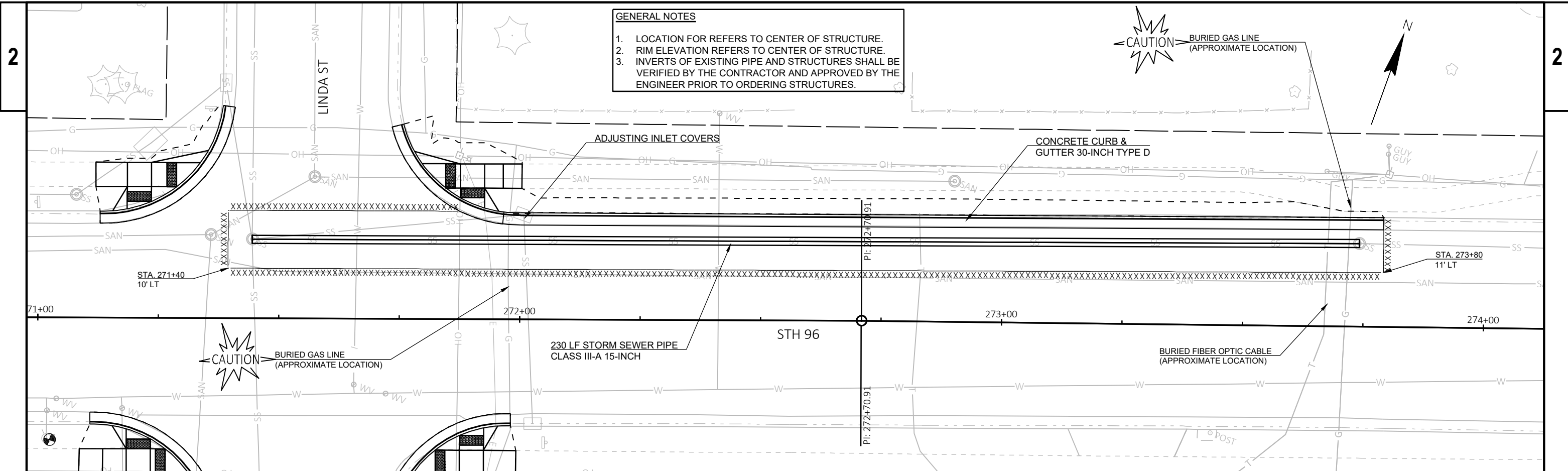
LEGEND	
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	SLOPE INTERCEPT
	INLET PROTECTION

GENERAL NOTES

1. LOCATION FOR REFERS TO CENTER OF STRUCTURE.
2. RIM ELEVATION REFERS TO CENTER OF STRUCTURE.
3. INVERTS OF EXISTING PIPE AND STRUCTURES SHALL BE VERIFIED BY THE CONTRACTOR AND APPROVED BY THE ENGINEER PRIOR TO ORDERING STRUCTURES.



PROJECT NO: 4075-35-72	HWY: STH 96	COUNTY: OUTAGAMIE	STORM SEWER - STORM SEWER	SHEET	E
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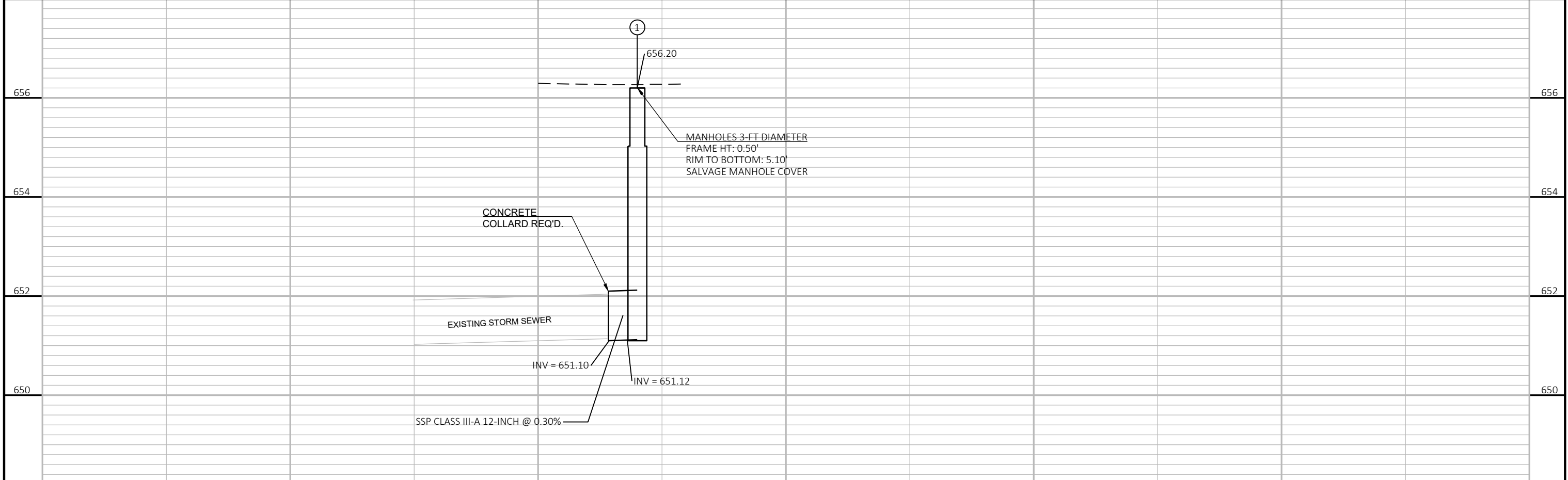
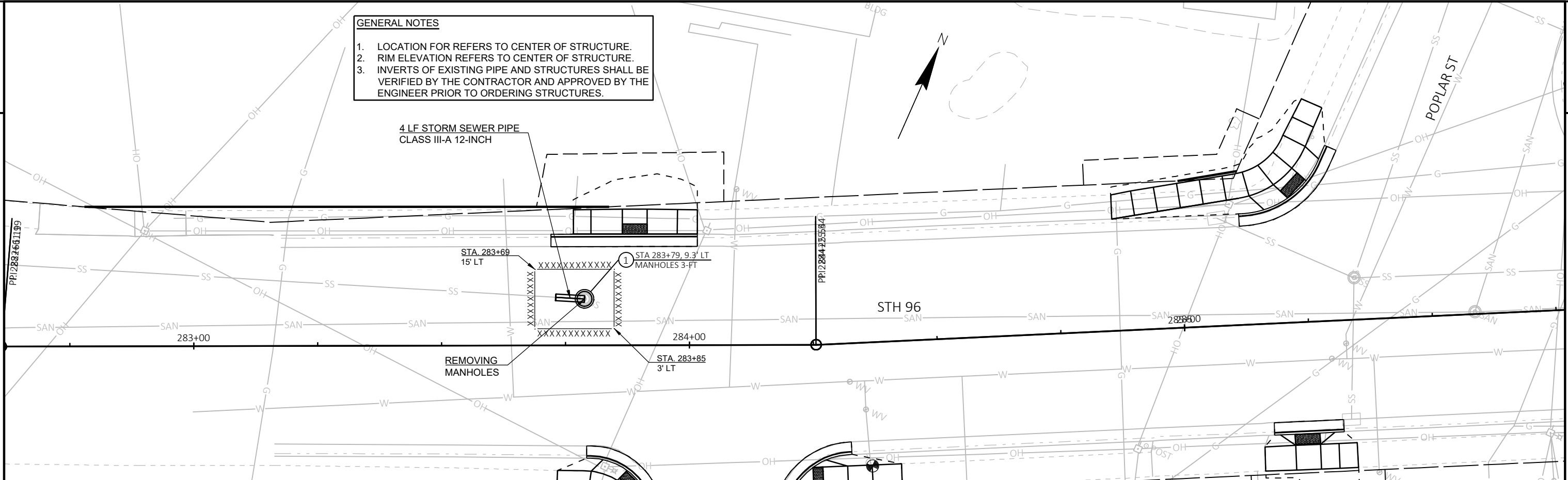


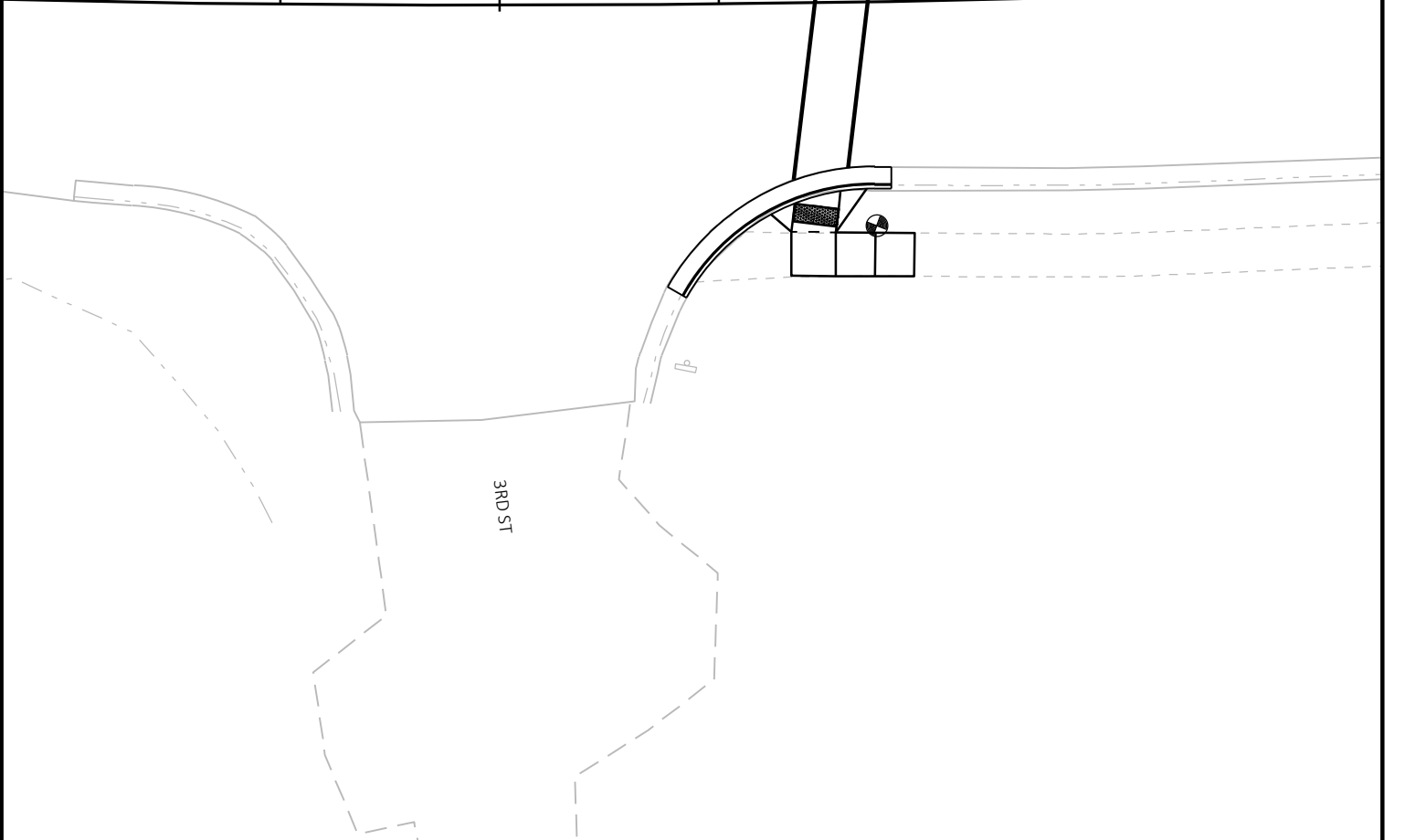
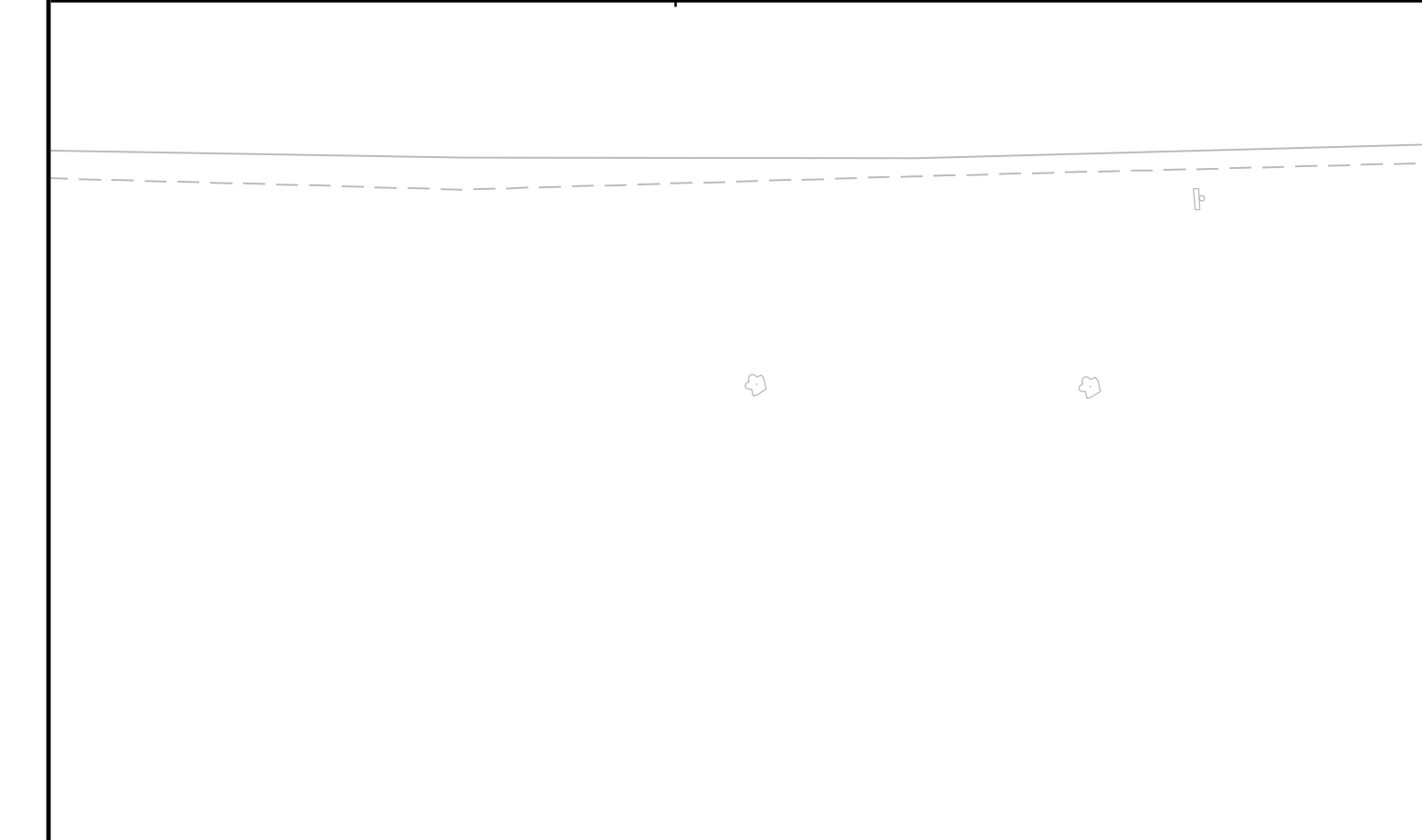
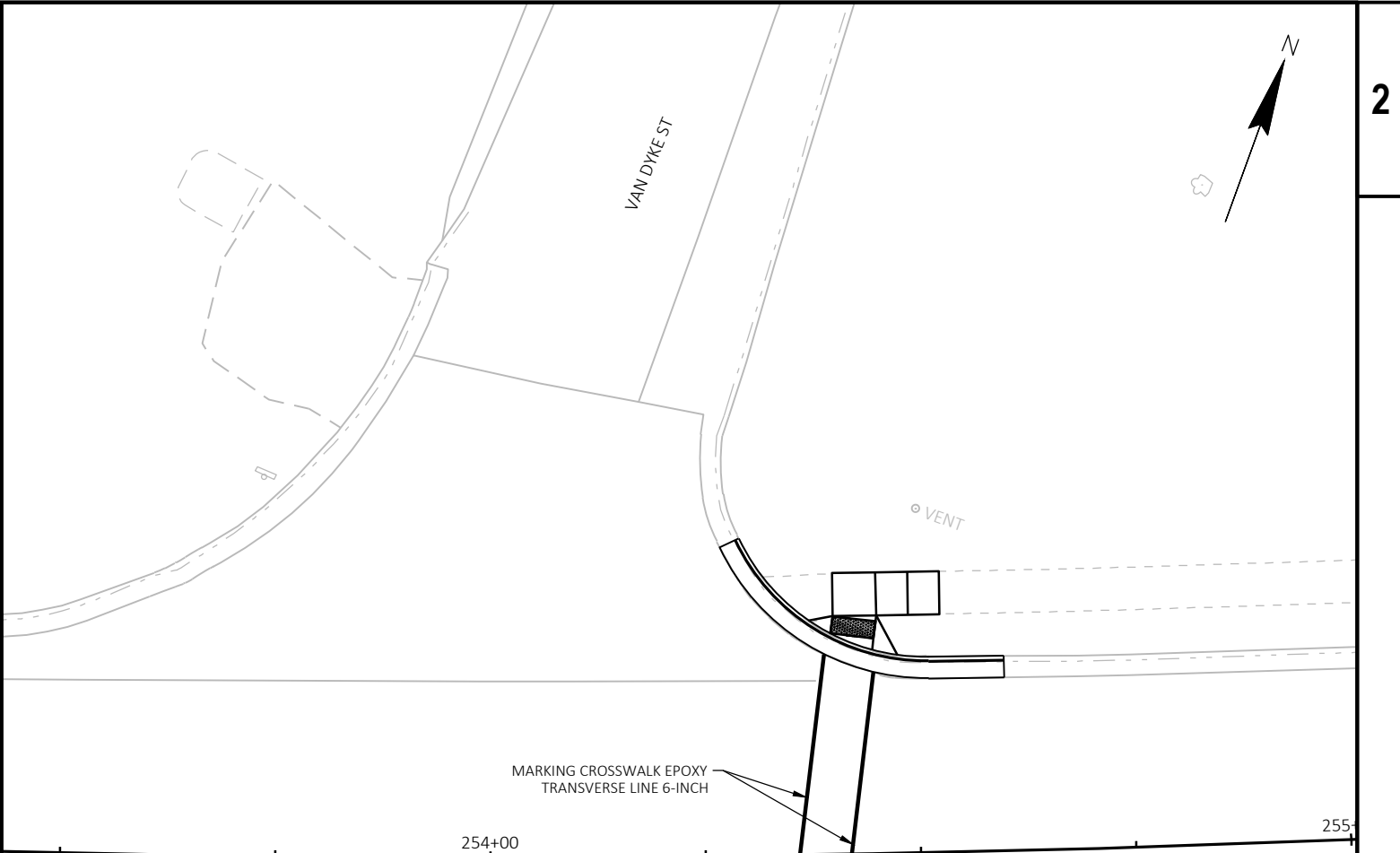
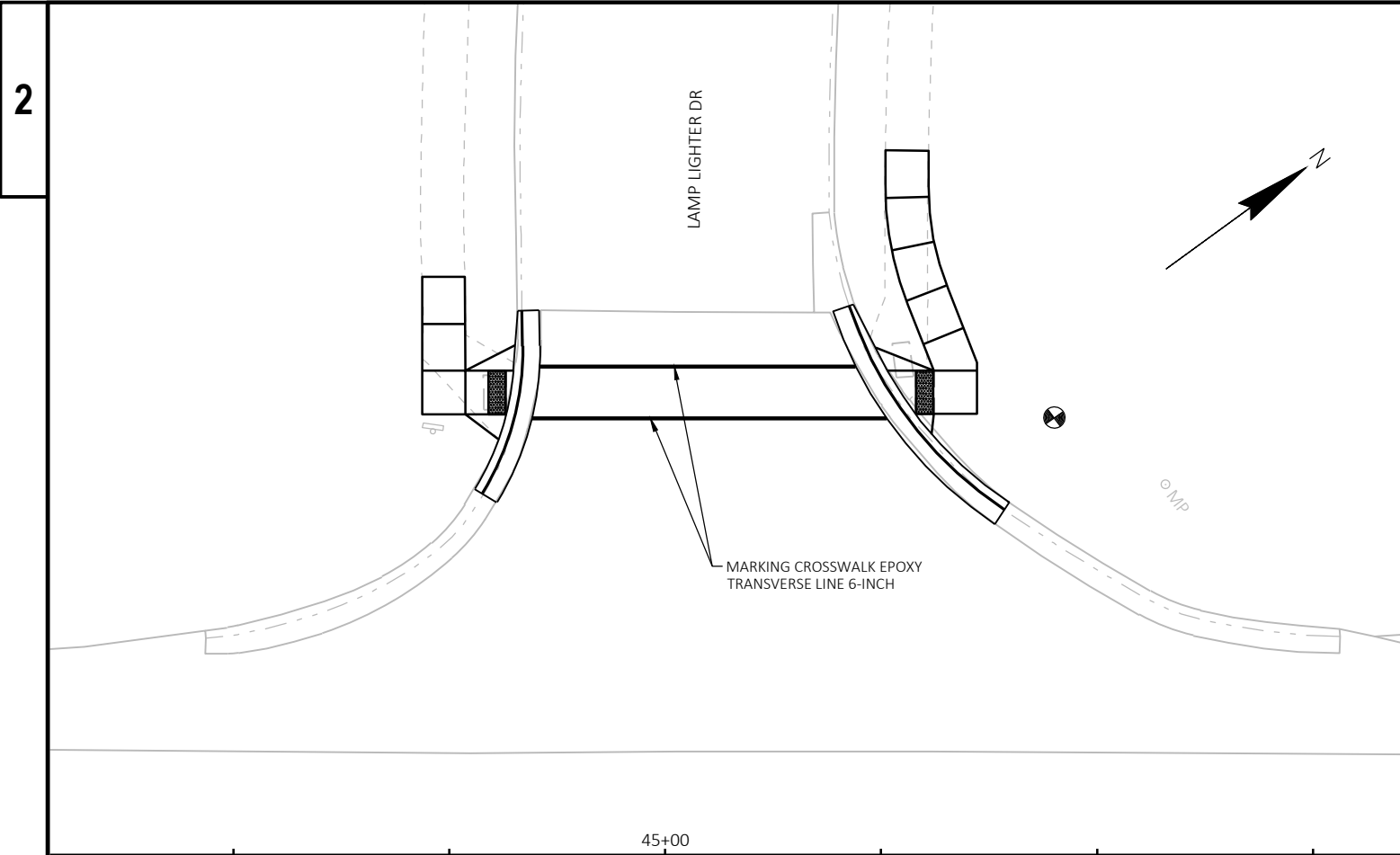
PROJECT NO: 4075-35-72	HWY: STH 96	COUNTY: OUTAGAMIE	STORM SEWER - STORM SEWER	SHEET	E
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GENERAL NOTES

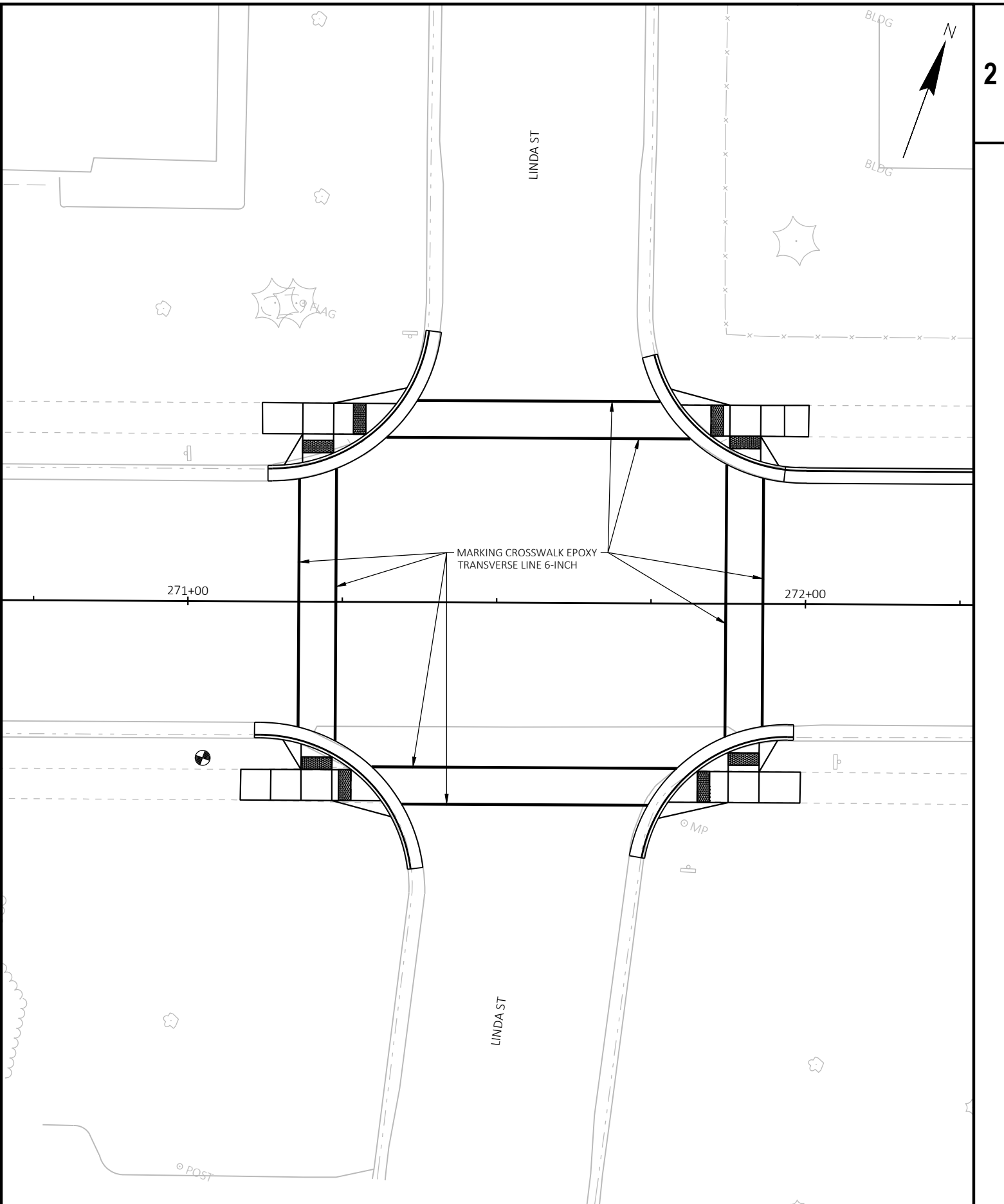
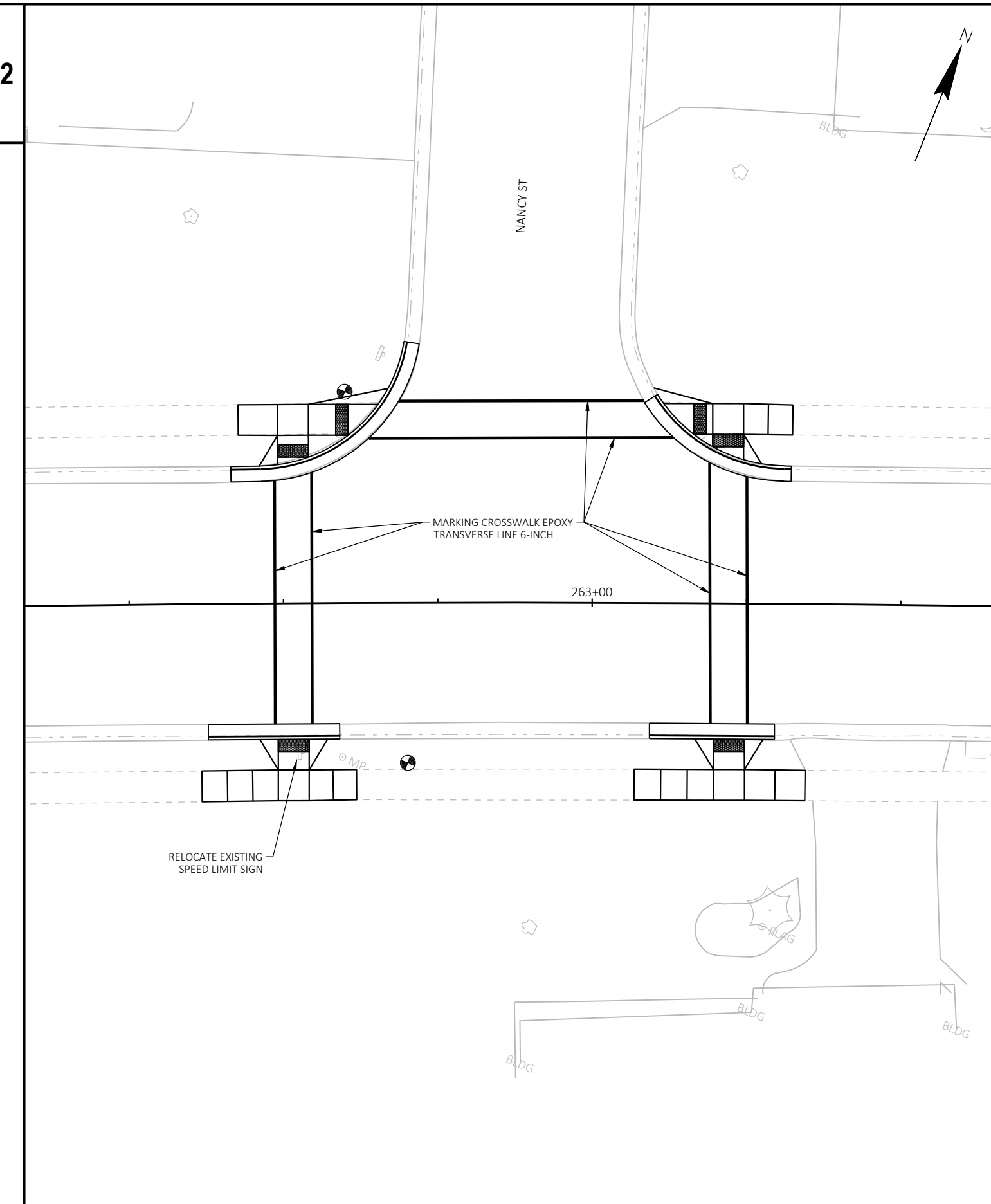
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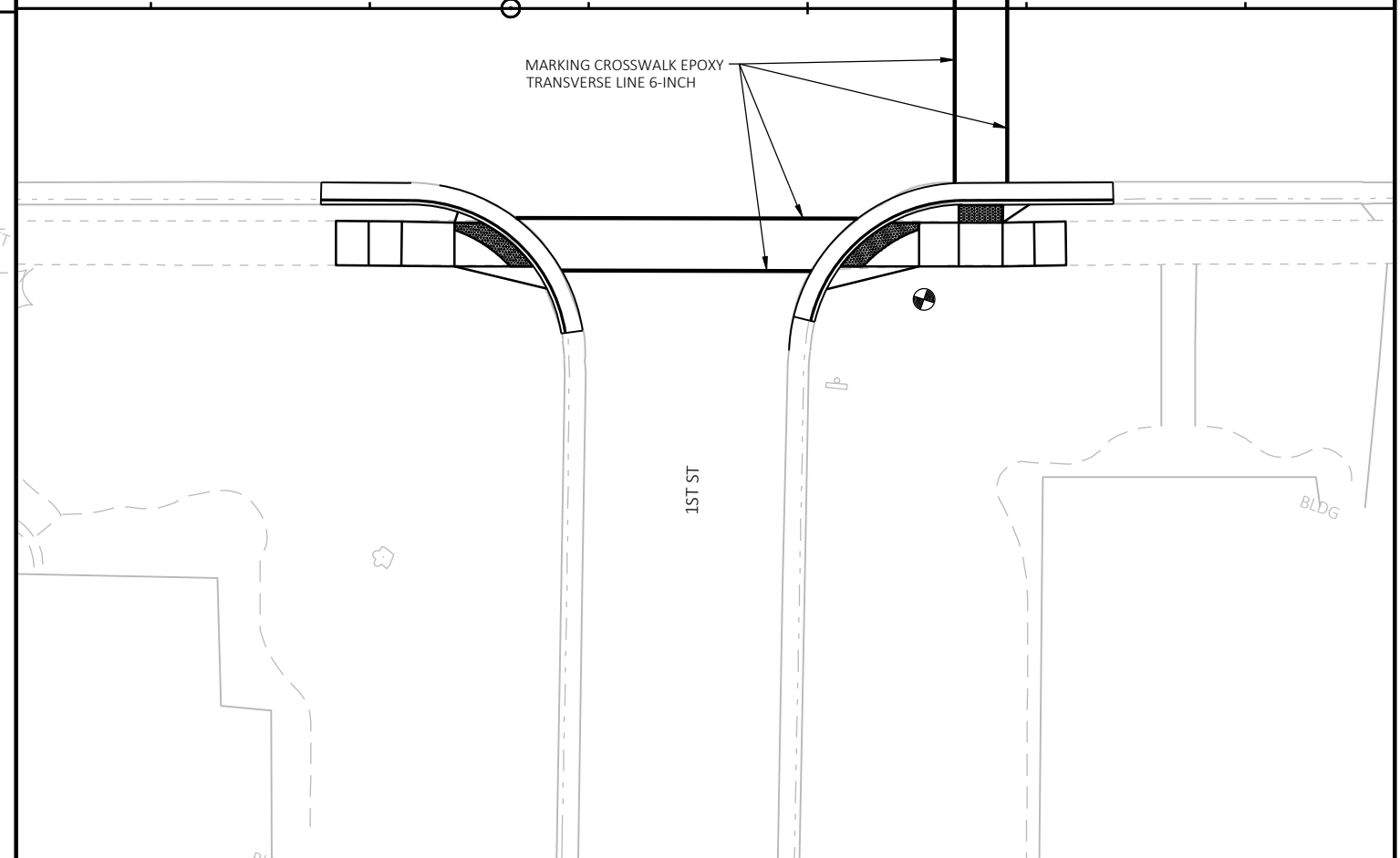
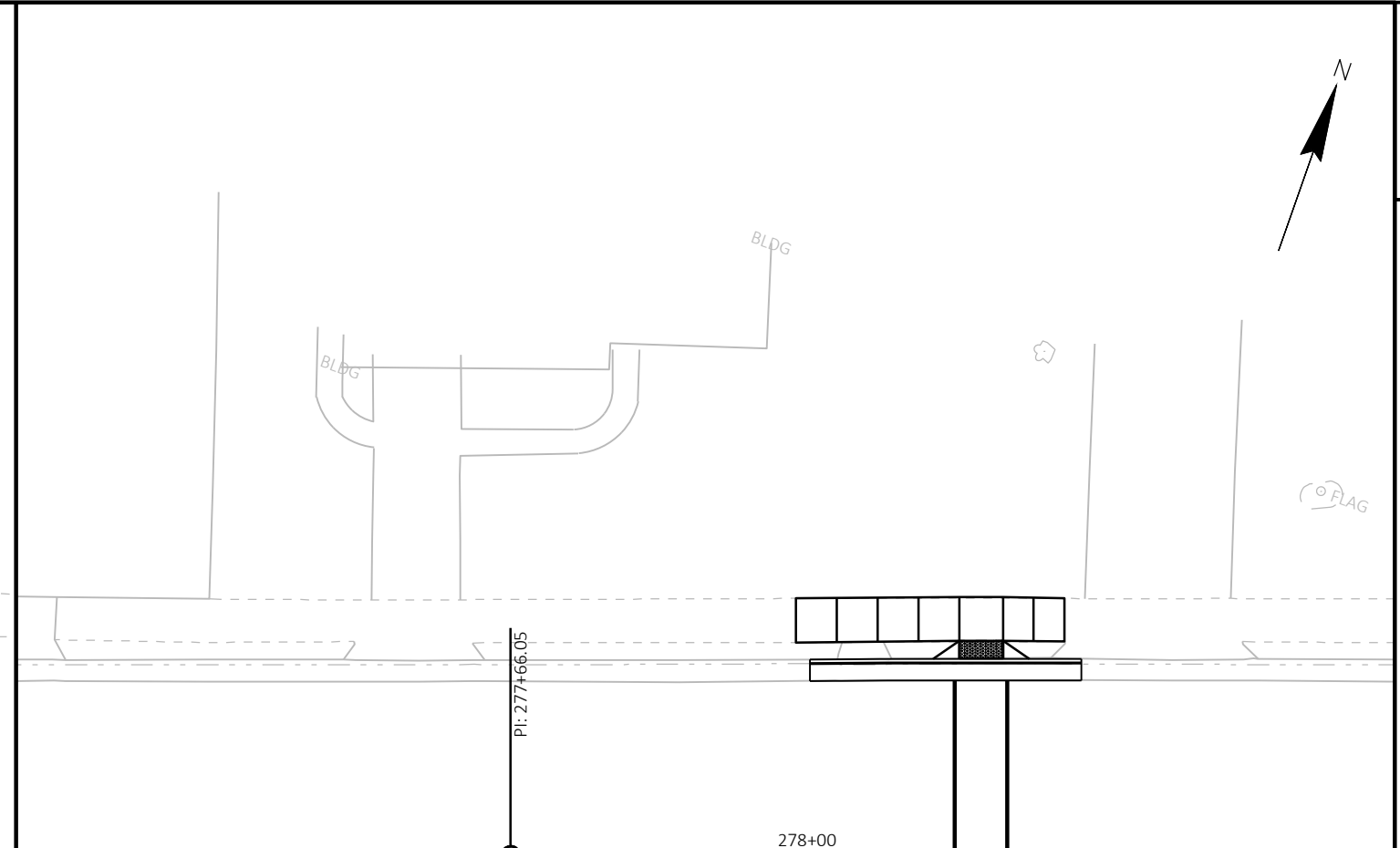
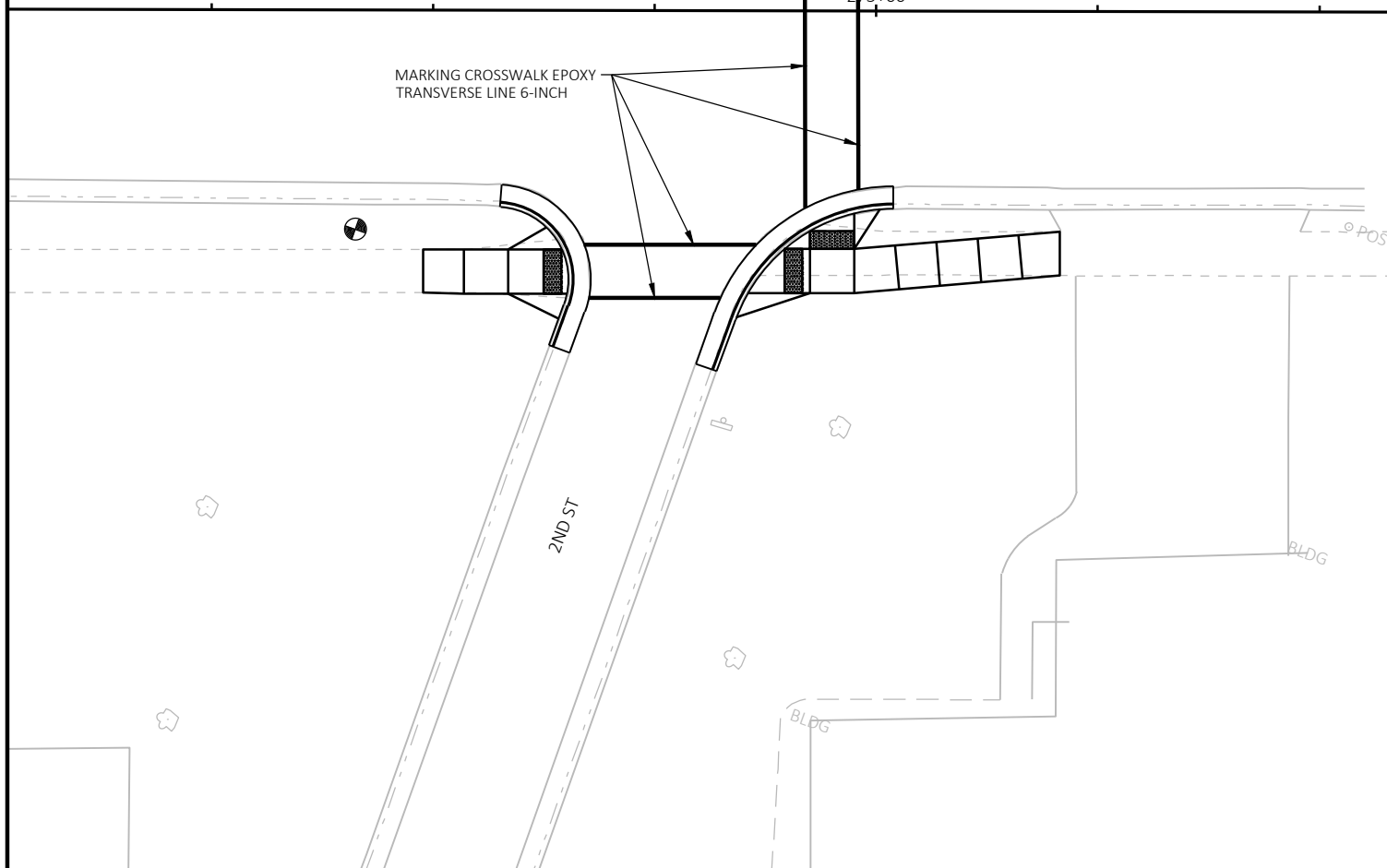
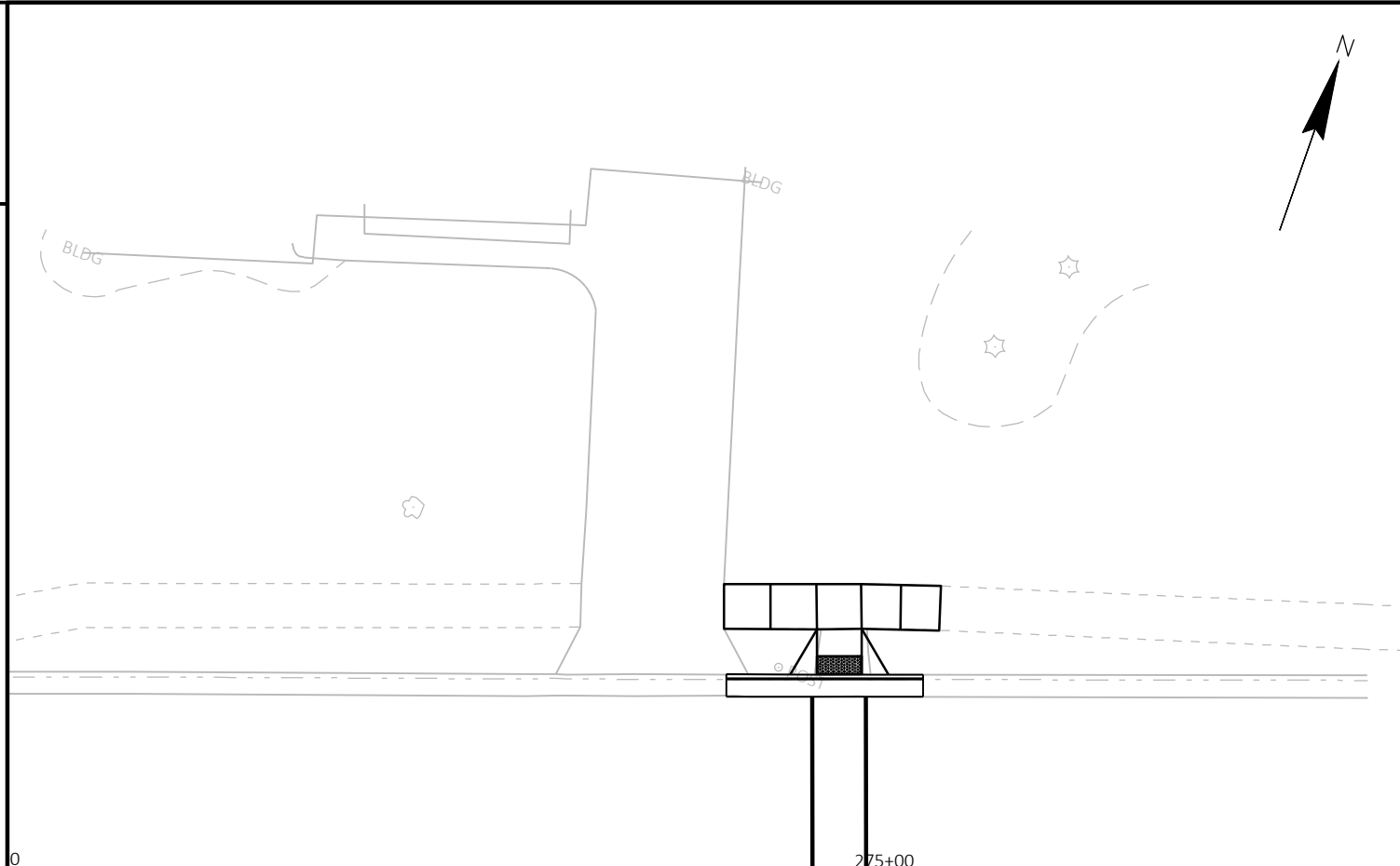
4 LF STORM SEWER PIPE
CLASS III-A 12-INCH



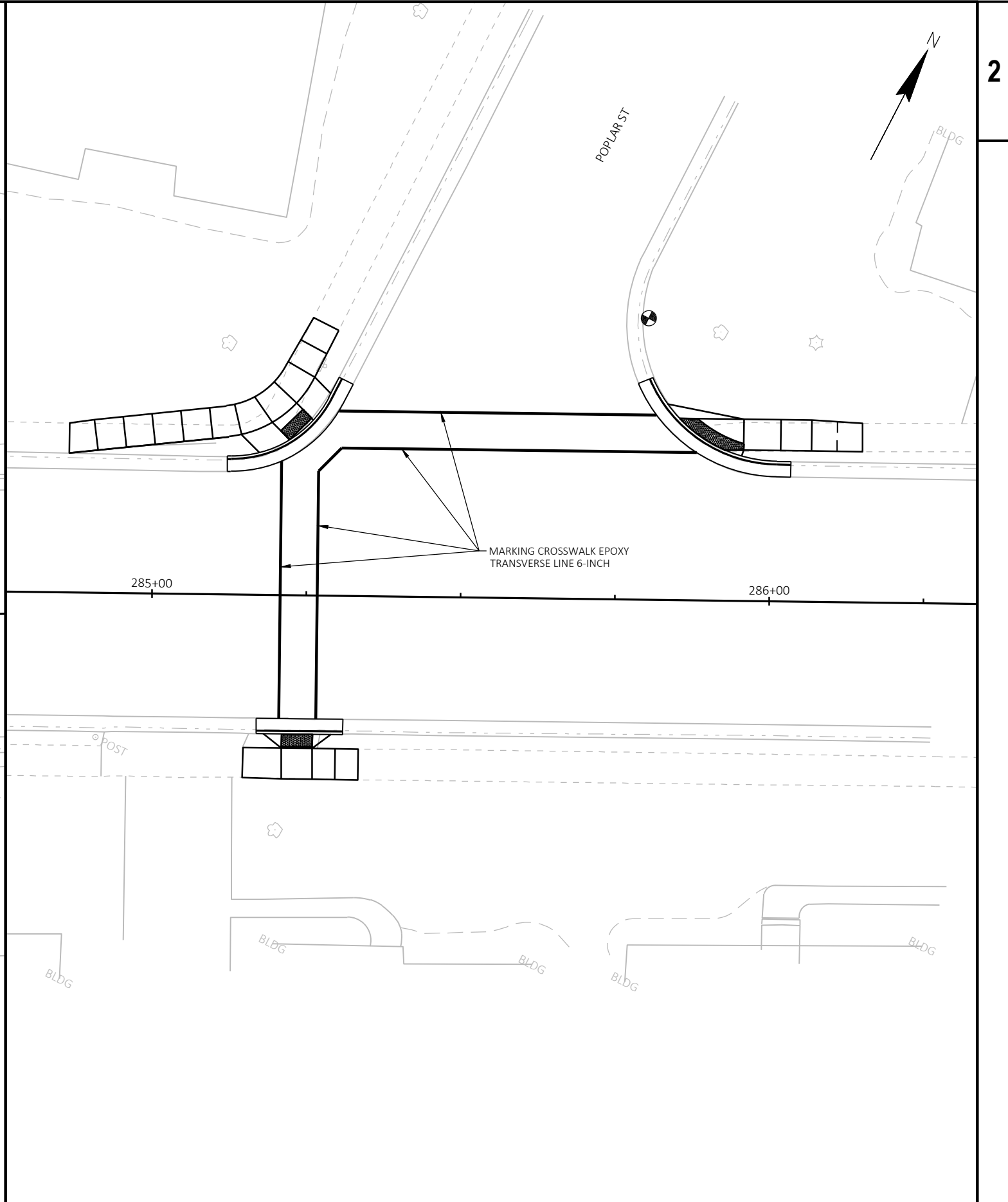
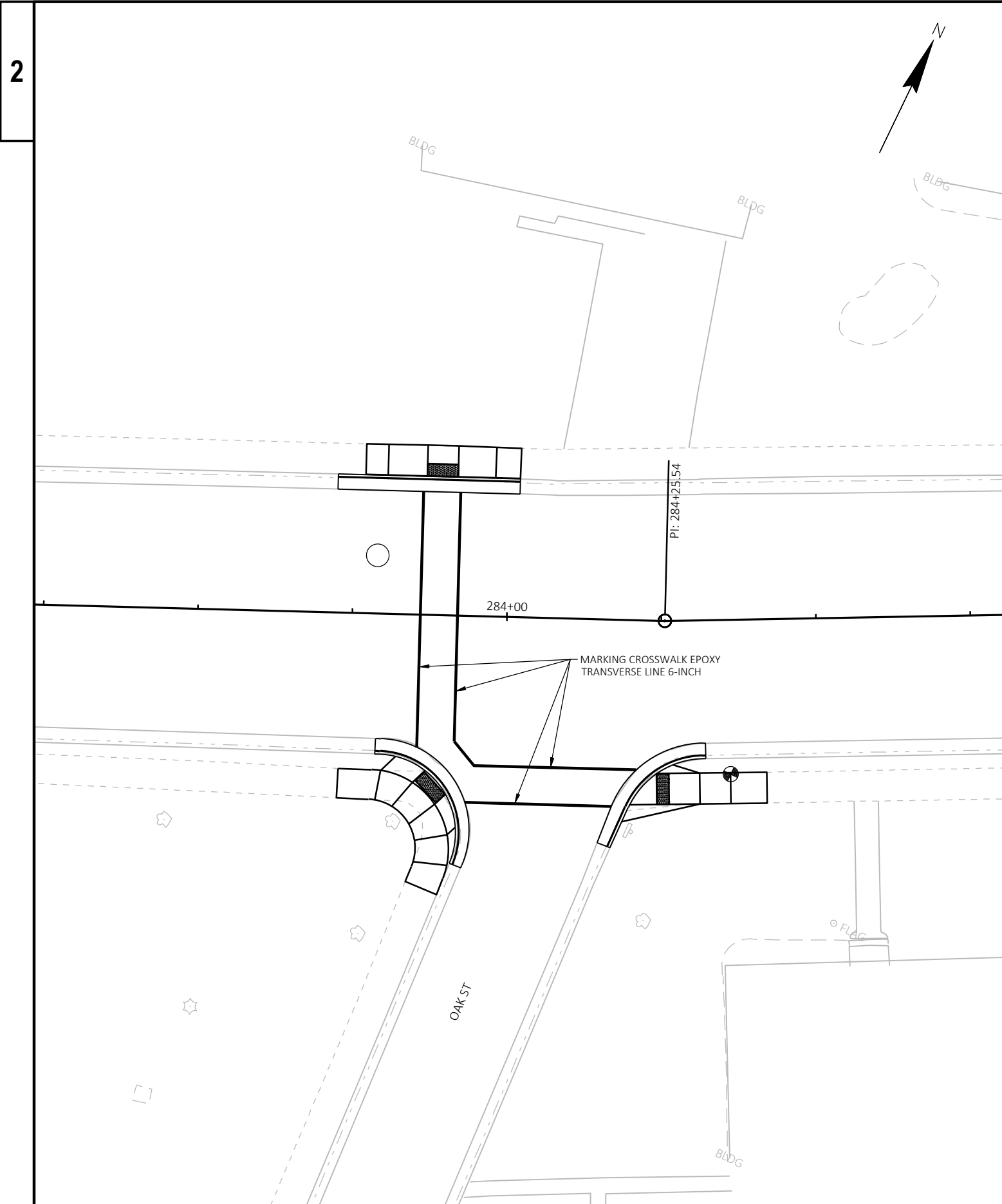


PROJECT NO: 4075-35-71/72	HWY: STH 96	COUNTY: BROWN & OUTAGAMIE	PAVEMENT MARKINGS	SHEET	E
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PROJECT NO: 4075-35-71/72	HWY: STH 96	COUNTY: BROWN & OUTAGAMIE	PAVEMENT MARKINGS	SHEET	E
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PROJECT NO: 4075-35-71/72

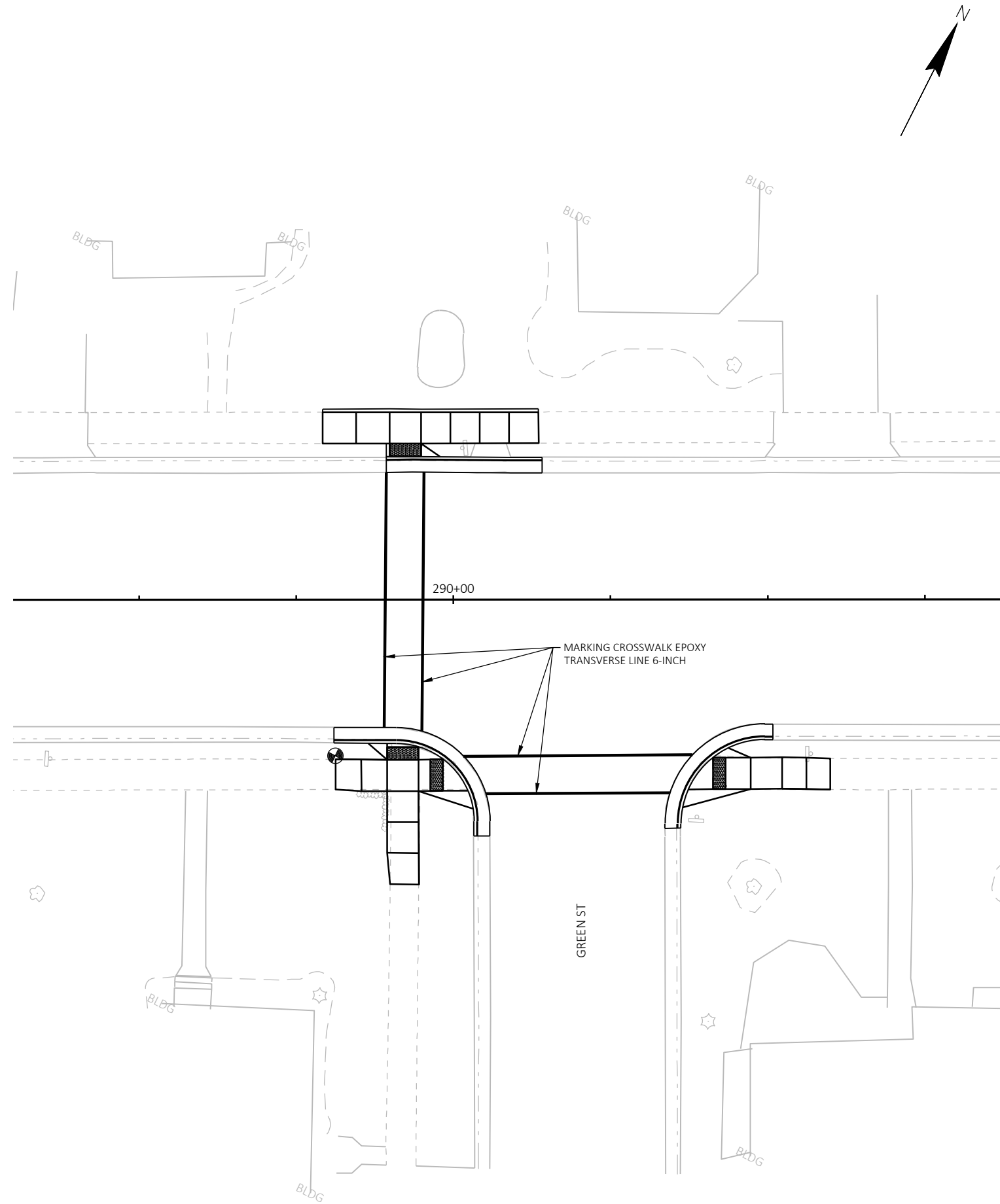
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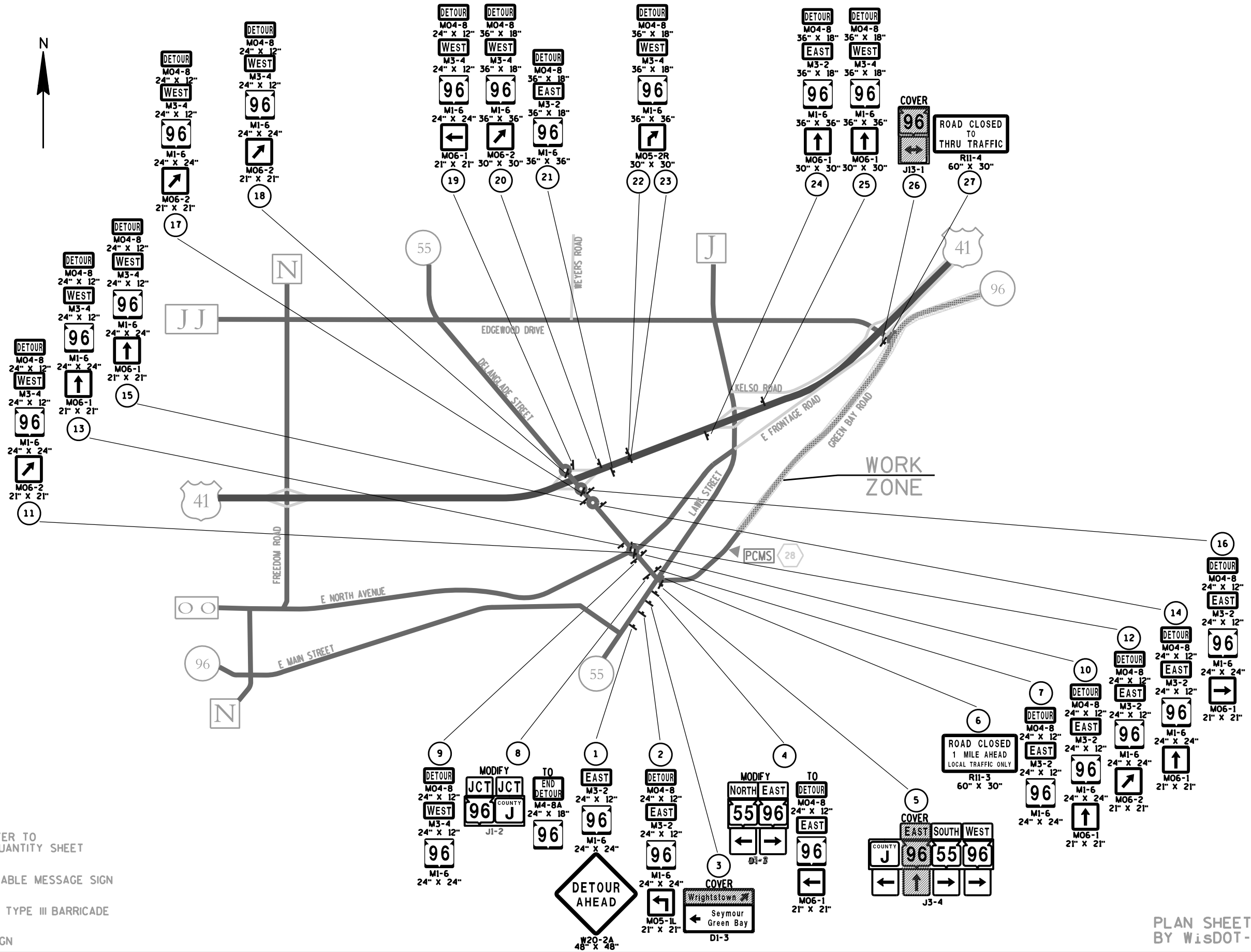
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PAVEMENT MARKINGS

SHEET

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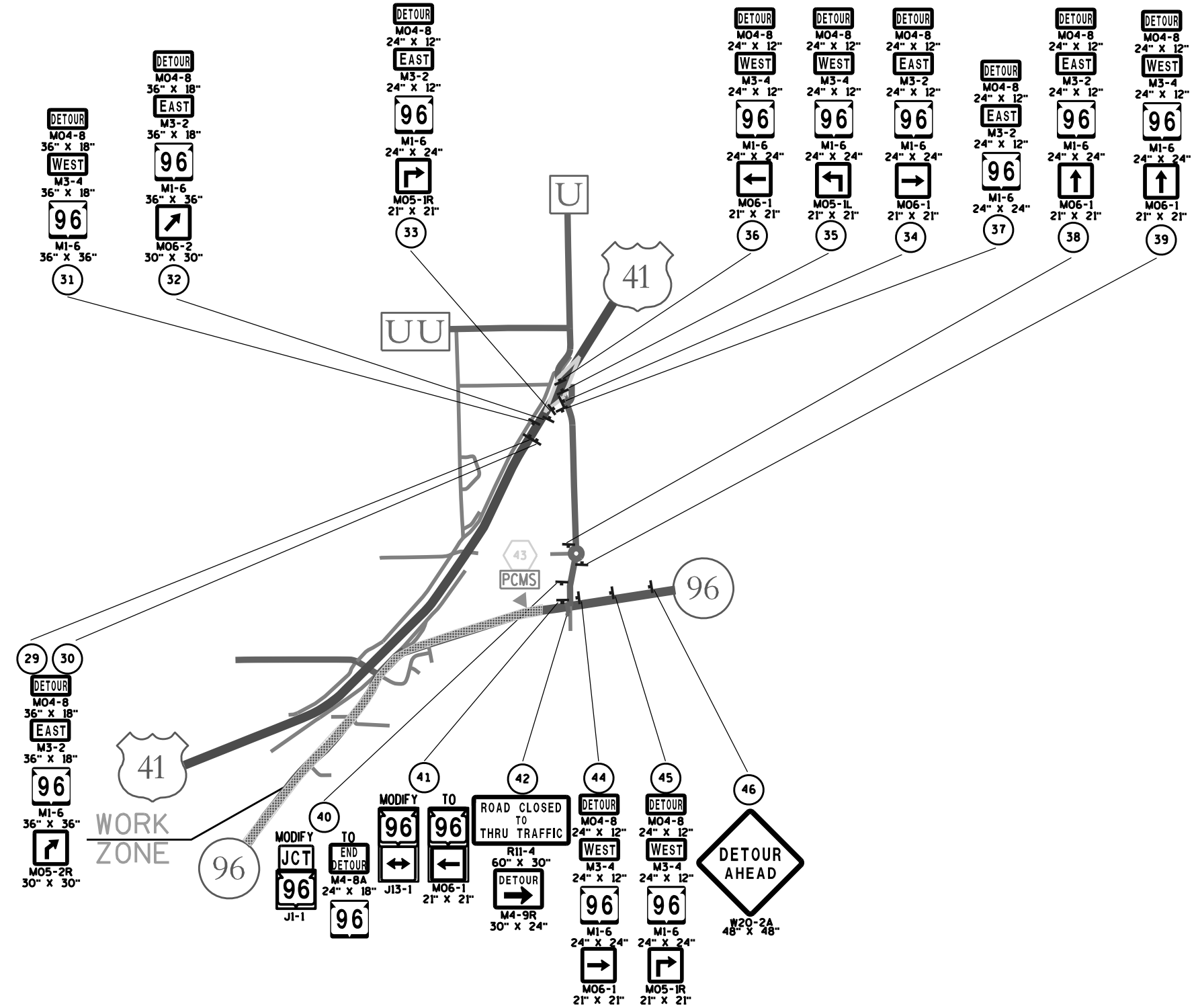




LEGEND

- (X) SIGN NUMBER. REFER TO MISCELLANEOUS QUANTITY SHEET
- (PCMS) (X) PORTABLE CHANGEABLE MESSAGE SIGN
- (BARRICADE) SIGN MOUNTED ON TYPE III BARRICADE
- (POST) POST MOUNTED SIGN

PLAN SHEET PRODUCED BY WisDOT-NE REGION



LEGEND

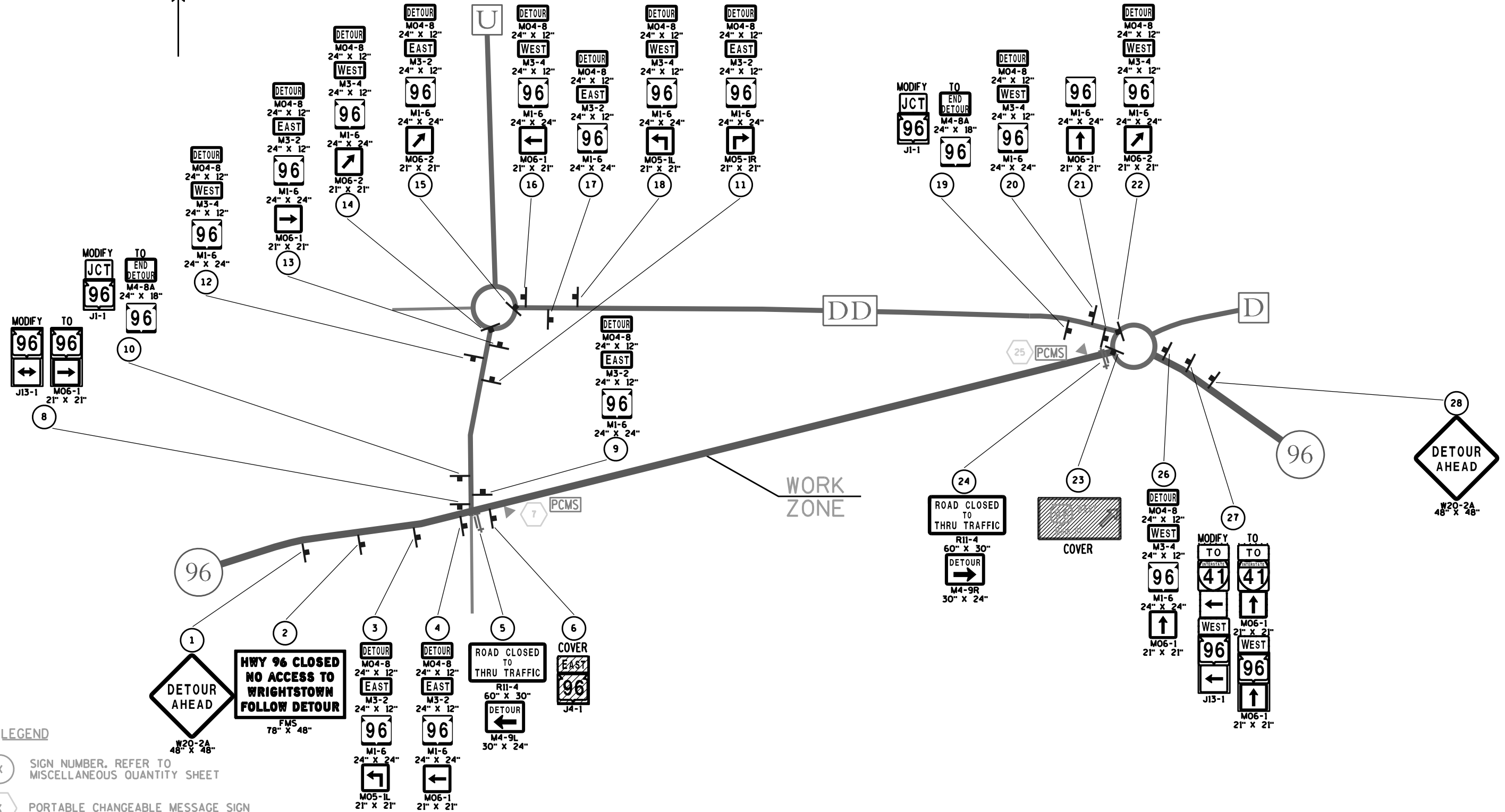
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(Symbol) SIGN MOUNTED ON TYPE III BARRICADE

(Symbol) POST MOUNTED SIGN




PLAN SHEET PRODUCED BY WisDOT-NE REGION









LEGEND

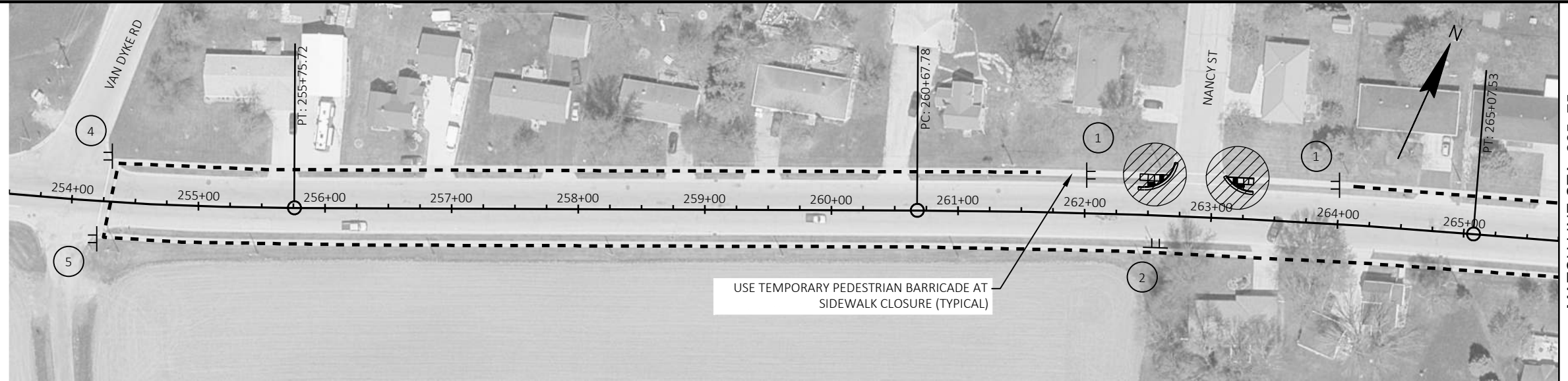
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- PCMS (X) PORTABLE CHANGEABLE MESSAGE SIGN
- ⇌ SIGN MOUNTED ON TYPE III BARRICADE
- POST MOUNTED SIGN

PLAN SHEET PRODUCED BY WisDOT-NE REGION

- LEGEND**
-  SIDEWALK WORK ZONE
 -  ACCESSIBLE PEDESTRIAN PATH
 -  SIGN ON TEMPORARY SUPPORT

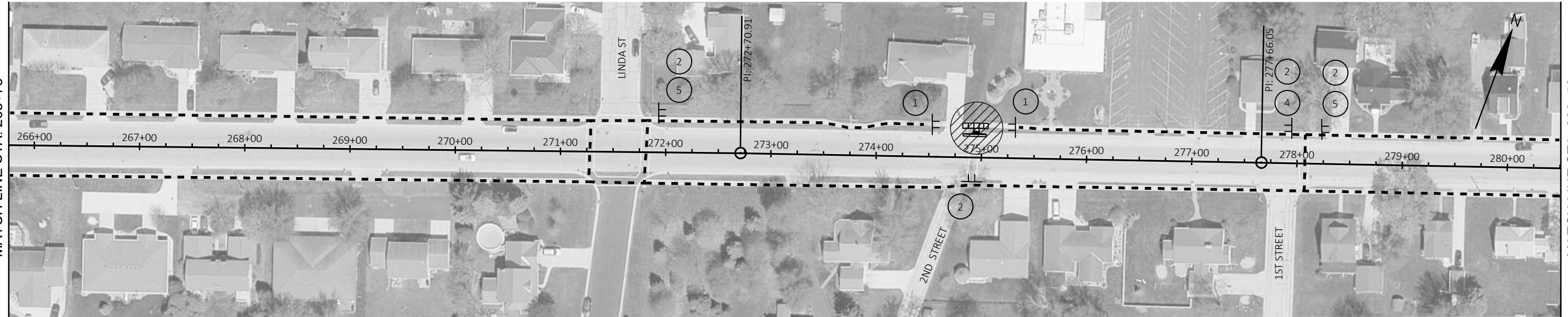
- | | | |
|--|--|--|
| 1 | 2 | 3 |
| 
R9-9
24"x12" | 
R9-9A
30"x18" | 
M4-9BA
30"x24" |
| 4 | 5 | 6 |
| 
M4-9BL
30"x24" | 
M4-9BR
30"x24" | 
M4-9RA
30"x24" |

SEE STANDARD DETAIL DRAWINGS FOR ADDITIONAL PEDESTRIAN TRAFFIC CONTROL DETAILS



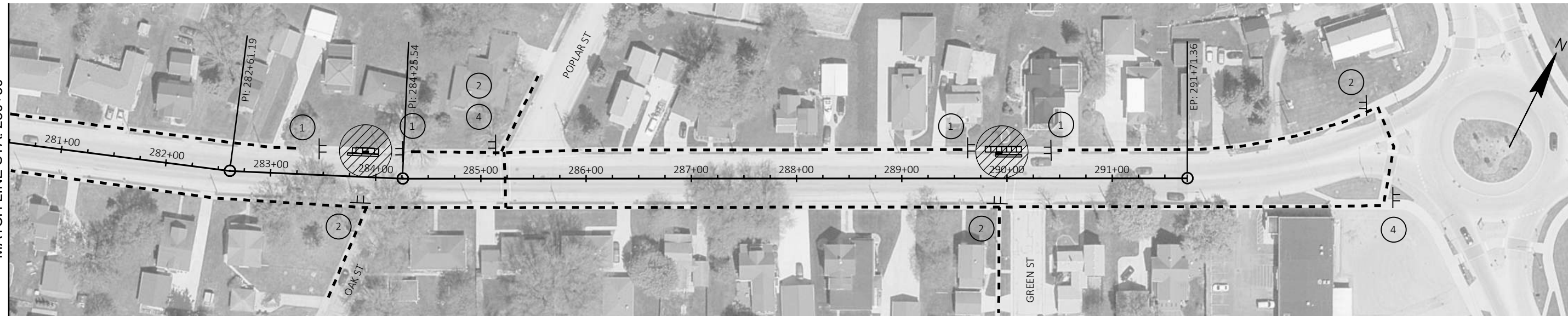
MATCH LINE STA. 265+75

MATCH LINE STA. 265+75



MATCH LINE STA. 280+50

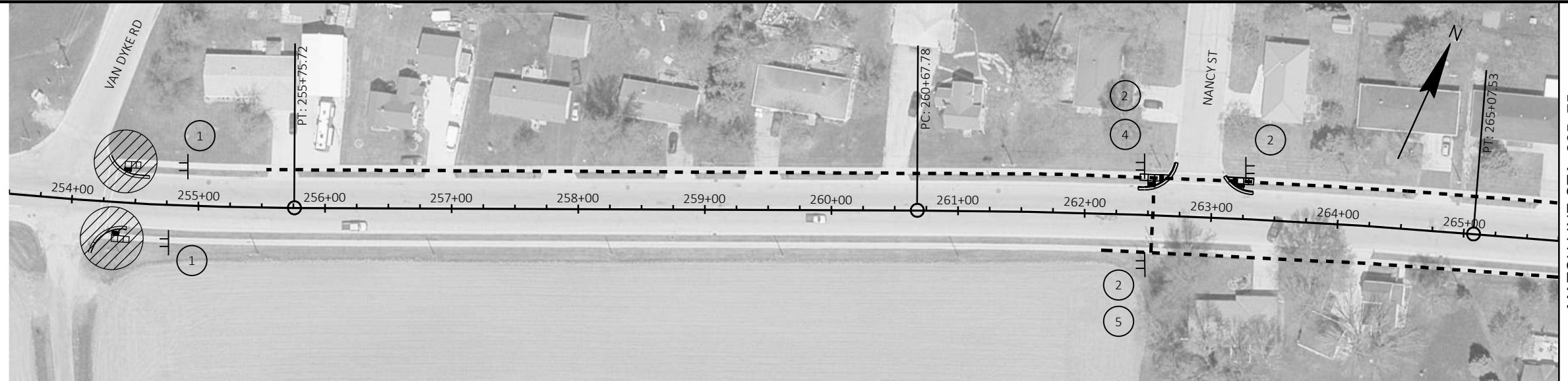
MATCH LINE STA. 280+50



- LEGEND**
- SIDEWALK WORK ZONE
 - ACCESSIBLE PEDESTRIAN PATH
 - SIGN ON TEMPORARY SUPPORT

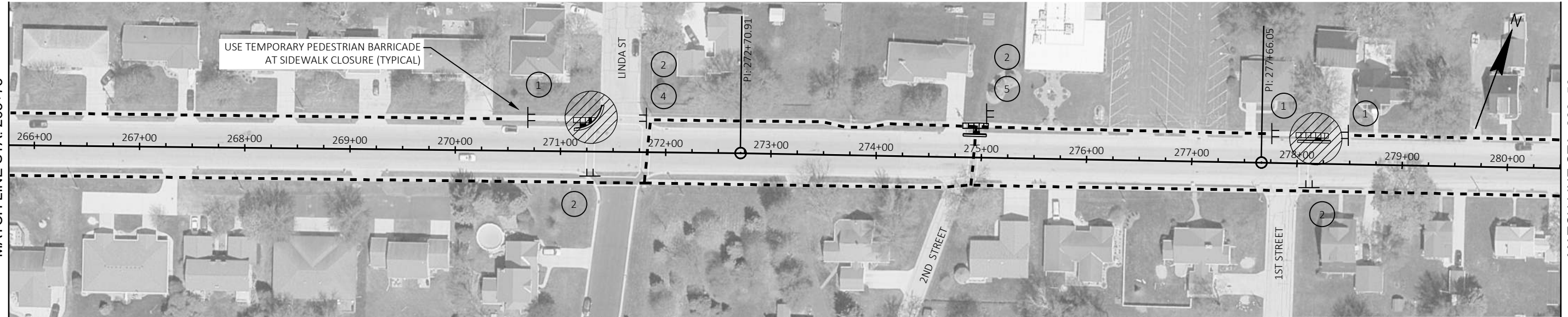
- | | | |
|---|---|---|
| 1 | 2 | 3 |
| | | |
| 4 | 5 | 6 |
| | | |

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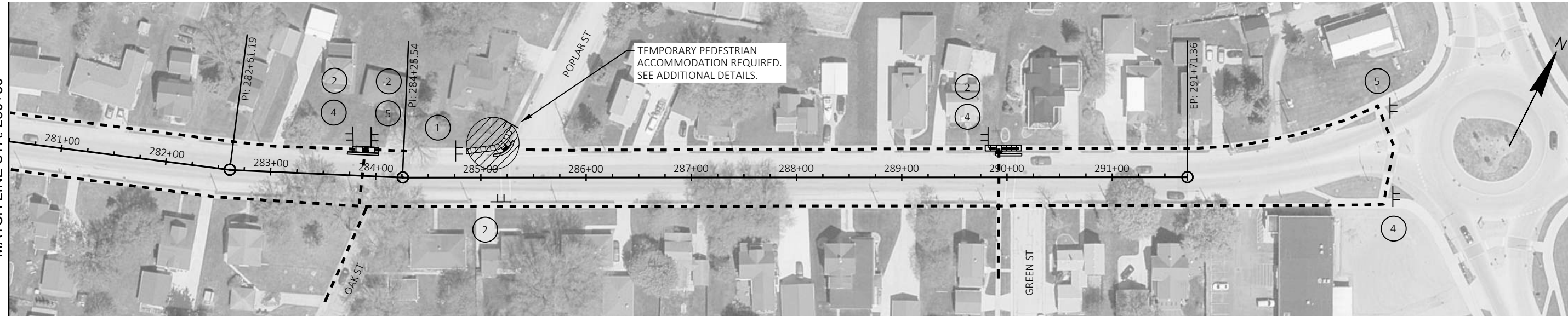
MATCH LINE STA. 265+75

MATCH LINE STA. 265+75










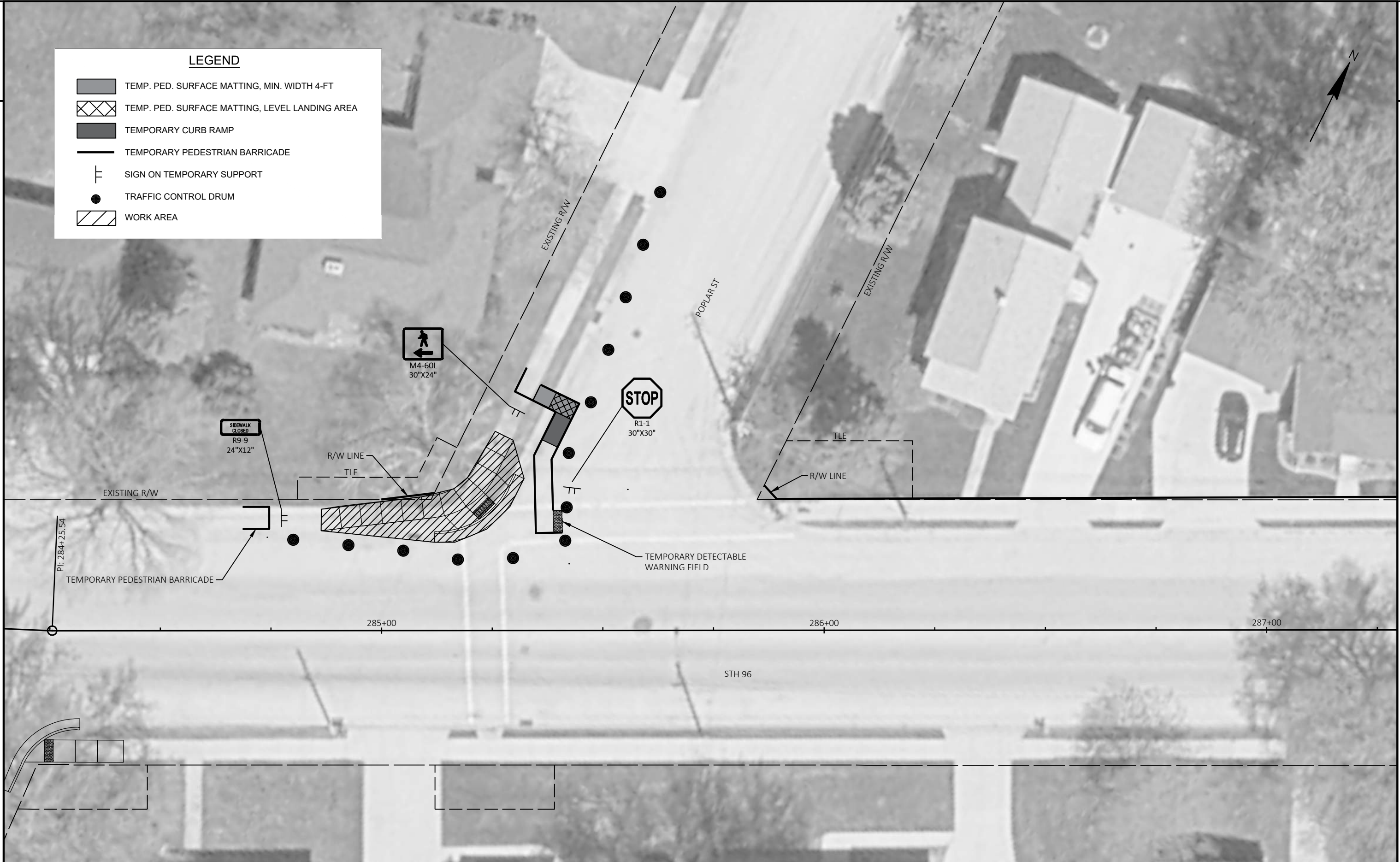
MATCH LINE STA. 280+50




MATCH LINE STA. 280+50









LEGEND

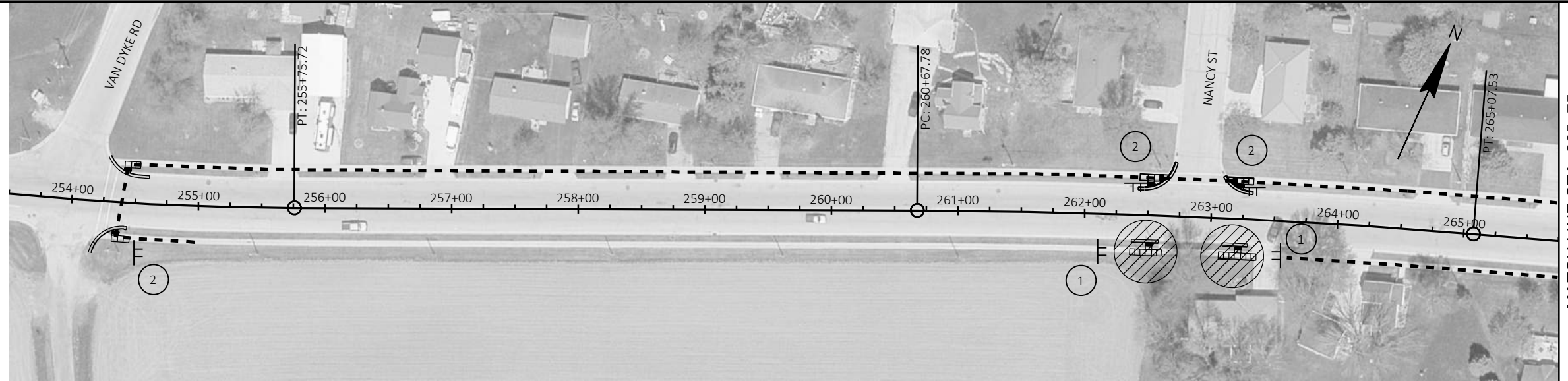
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-  TEMP. PED. SURFACE MATTING, LEVEL LANDING AREA
-  TEMPORARY CURB RAMP
-  TEMPORARY PEDESTRIAN BARRICADE
-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL DRUM
-  WORK AREA



- LEGEND**
-  SIDEWALK WORK ZONE
 -  ACCESSIBLE PEDESTRIAN PATH
 -  SIGN ON TEMPORARY SUPPORT

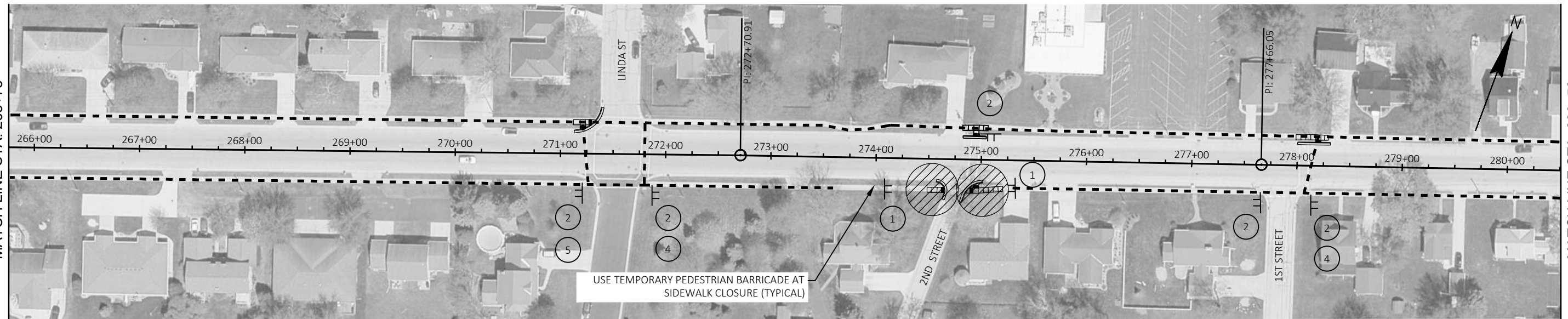
- | | | |
|---|---|---|
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| 4 | 5 | 6 |
|  |  |  |

SEE STANDARD DETAIL DRAWINGS FOR ADDITIONAL PEDESTRIAN TRAFFIC CONTROL DETAILS



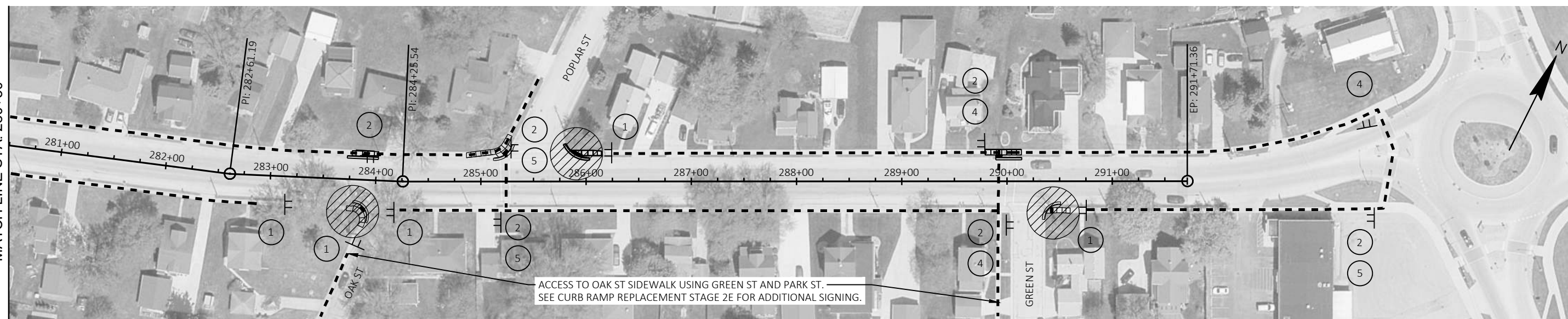
MATCH LINE STA. 265+75




MATCH LINE STA. 265+75









MATCH LINE STA. 280+50

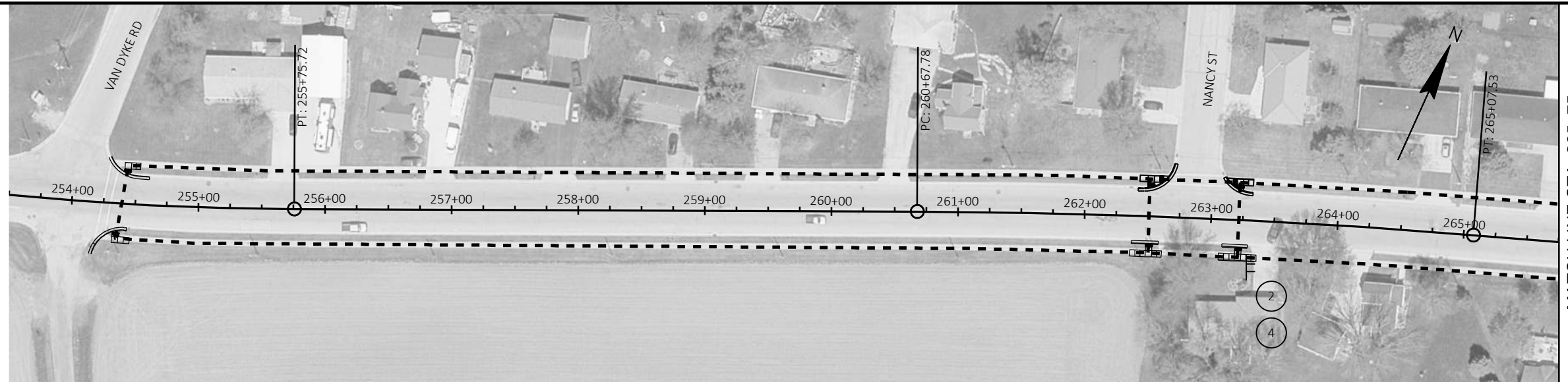
MATCH LINE STA. 280+50



- LEGEND**
-  SIDEWALK WORK ZONE
 -  ACCESSIBLE PEDESTRIAN PATH
 -  SIGN ON TEMPORARY SUPPORT

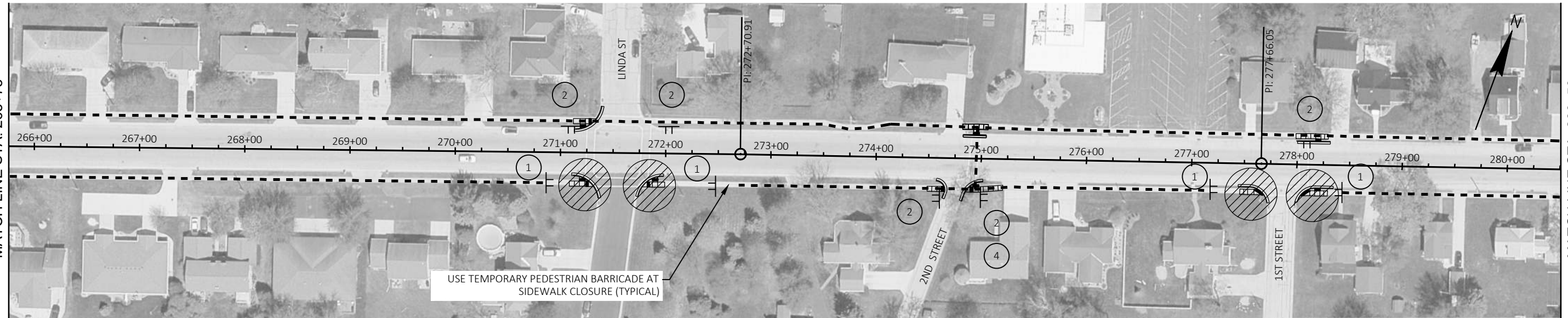
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| 1 | 2 | 3 |
| 
R9-9
24"x12" | 
R9-9A
30"x18" | 
M4-9BA
30"x24" |
| 4 | 5 | 6 |
| 
M4-9BL
30"x24" | 
M4-9BR
30"x24" | 
M4-9RA
30"x24" |

SEE STANDARD DETAIL DRAWINGS FOR ADDITIONAL PEDESTRIAN TRAFFIC CONTROL DETAILS



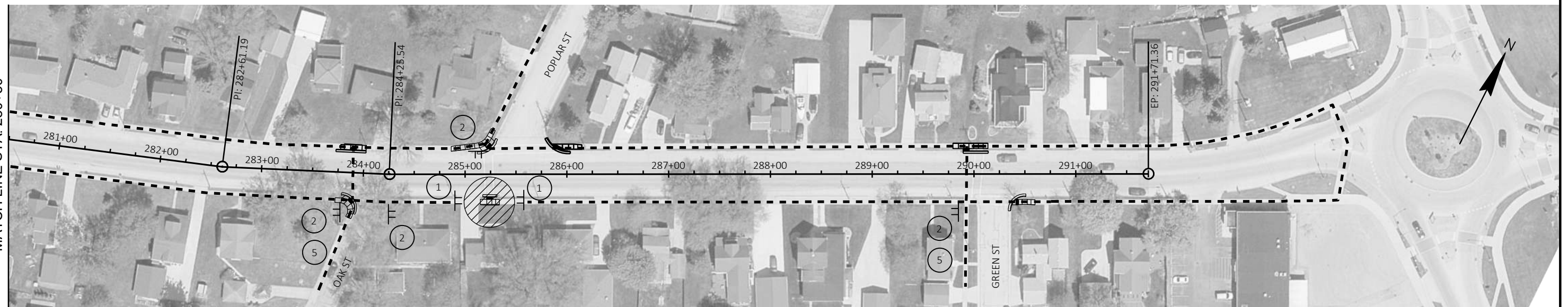
MATCH LINE STA. 265+75

MATCH LINE STA. 265+75






MATCH LINE STA. 280+50

MATCH LINE STA. 280+50

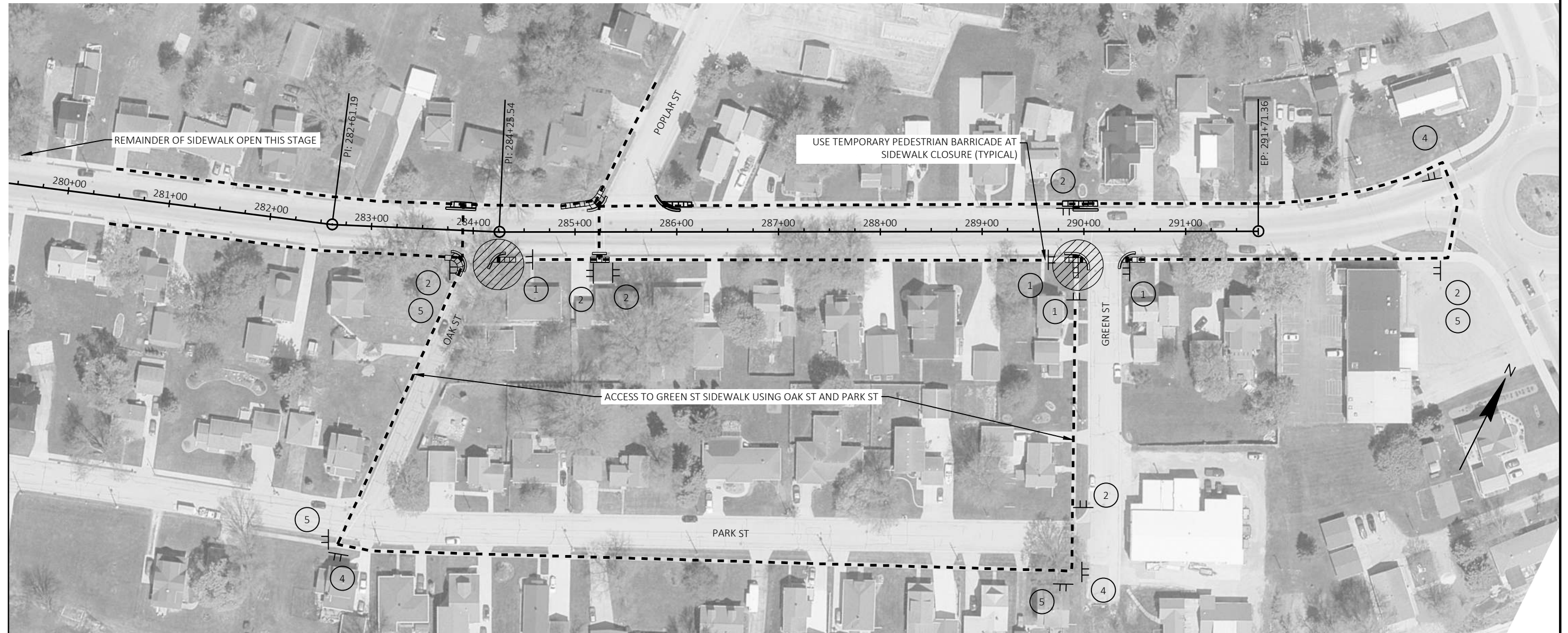


LEGEND

-  SIDEWALK WORK ZONE
-  ACCESSIBLE PEDESTRIAN PATH
-  SIGN ON TEMPORARY SUPPORT




- | | | |
|---|---|---|
| 1 | 2 | 3 |
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SEE STANDARD DETAIL DRAWINGS FOR ADDITIONAL PEDESTRIAN TRAFFIC CONTROL DETAILS



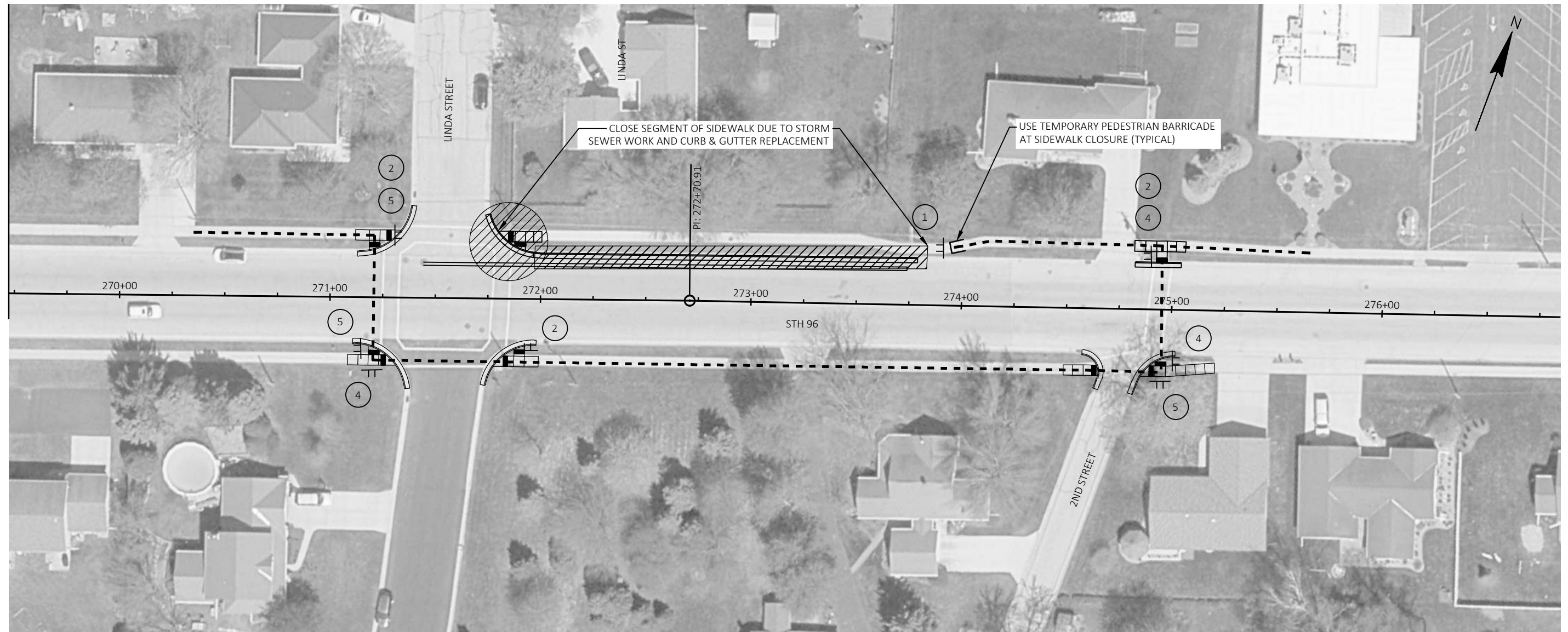
PROJECT NO: 4075-35-72	HWY: STH 96	COUNTY: BROWN	CURB RAMP REPLACEMENT - STAGE 2E	SHEET	E
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LEGEND

-  SIDEWALK AND CURB WORK ZONE
-  ACCESSIBLE PEDESTRIAN PATH
-  SIGN ON TEMPORARY SUPPORT

- | | | |
|---|---|---|
| ① | ② | ③ |
| | | |
| ④ | ⑤ | ⑥ |
| | | |

SEE STANDARD DETAIL DRAWINGS FOR ADDITIONAL PEDESTRIAN TRAFFIC CONTROL DETAILS



Estimate Of Quantities

Line	Item	Item Description	Unit	4075-35-71		4075-35-72	
				Total	Qty	Total	Qty
0002	201.0205	Grubbing	STA	3.000		3.000	
0004	201.0220	Grubbing	ID	12.000		12.000	
0006	203.0100	Removing Small Pipe Culverts	EACH	6.000	2.000	4.000	
0008	204.0110	Removing Asphaltic Surface	SY	2,700.000	405.000	2,295.000	
0010	204.0115	Removing Asphaltic Surface Butt Joints	SY	1,650.000	283.000	1,367.000	
0012	204.0120	Removing Asphaltic Surface Milling	SY	103,000.000	34,785.000	68,215.000	
0014	204.0150	Removing Curb & Gutter	LF	960.000	53.000	907.000	
0016	204.0155	Removing Concrete Sidewalk	SY	395.000	28.000	367.000	
0018	204.0210	Removing Manholes	EACH	1.000		1.000	
0020	204.0220	Removing Inlets	EACH	2.000		2.000	
0022	204.0245	Removing Storm Sewer (size) 01. 12-Inch	LF	21.000		21.000	
0024	204.0245	Removing Storm Sewer (size) 02. 15-Inch	LF	234.000		234.000	
0026	204.0245	Removing Storm Sewer (size) 03. 24-Inch	LF	143.000		143.000	
0028	204.0245	Removing Storm Sewer (size) 04. 30-Inch	LF	56.000		56.000	
0030	204.0270	Abandoning Culvert Pipes	EACH	1.000		1.000	
0032	204.9060.S	Removing (item description) 01. Removing Apron Endwalls	EACH	2.000		2.000	
0034	205.0100	Excavation Common	CY	1,630.000	295.000	1,335.000	
0036	208.0100	Borrow	CY	80.000		80.000	
0038	211.0101	Prepare Foundation for Asphaltic Paving (project) 01. 4075-35-71	EACH	1.000	1.000		
0040	211.0101	Prepare Foundation for Asphaltic Paving (project) 02. 4075-35-72	EACH	1.000		1.000	
0042	211.0400	Prepare Foundation for Asphaltic Shoulders	STA	170.000	170.000		
0044	213.0100	Finishing Roadway (project) 01. 4075-35-71	EACH	1.000	1.000		
0046	213.0100	Finishing Roadway (project) 02. 4075-35-72	EACH	1.000		1.000	
0048	305.0110	Base Aggregate Dense 3/4-Inch	TON	2,700.000	1,165.000	1,535.000	
0050	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	3,110.000	533.000	2,577.000	
0052	416.0610	Drilled Tie Bars	EACH	124.000	16.000	108.000	
0054	455.0605	Tack Coat	GAL	9,668.000	2,272.000	7,396.000	
0056	460.0105.S	HMA Percent Within Limits (PWL) Test Strip Volumetrics	EACH	2.000	1.000	1.000	
0058	460.0110.S	HMA Percent Within Limits (PWL) Test Strip Density	EACH	2.000	1.000	1.000	
0060	460.2000	Incentive Density HMA Pavement	DOL	4,160.000		4,160.000	
0062	460.2005	Incentive Density PWL HMA Pavement	DOL	7,676.000	2,686.000	4,990.000	
0064	460.2007	Incentive Density HMA Pavement Longitudinal Joints	DOL	25,821.000	9,554.000	16,267.000	
0066	460.2010	Incentive Air Voids HMA Pavement	DOL	11,318.000	4,250.000	7,068.000	
0068	460.6223	HMA Pavement 3 MT 58-28 S	TON	6,485.000		6,485.000	
0070	460.6224	HMA Pavement 4 MT 58-28 S	TON	11,318.000	4,250.000	7,068.000	
0072	465.0105	Asphaltic Surface	TON	1,094.000	370.000	724.000	
0074	465.0110	Asphaltic Surface Patching	TON	75.000	5.000	70.000	
0076	465.0120	Asphaltic Surface Driveways and Field Entrances	TON	90.000	60.000	30.000	
0078	465.0315	Asphaltic Flumes	SY	5.000		5.000	
0080	465.0520	Asphaltic Rumble Strips, Shoulder	LF	29,180.000	8,860.000	20,320.000	
0082	465.0560	Asphaltic Rumble Strips, Centerline	LF	16,200.000	5,350.000	10,850.000	
0084	520.1030	Apron Endwalls for Culvert Pipe 30-Inch	EACH	4.000		4.000	
0086	520.1036	Apron Endwalls for Culvert Pipe 36-Inch	EACH	4.000	2.000	2.000	
0088	520.3430	Culvert Pipe Class III-A Non-metal 30-Inch	LF	234.000		234.000	
0090	520.3436	Culvert Pipe Class III-A Non-metal 36-Inch	LF	166.000	60.000	106.000	
0092	520.8000	Concrete Collars for Pipe	EACH	1.000		1.000	
0094	520.8700	Cleaning Culvert Pipes	EACH	1.000	1.000		
0096	521.1024	Apron Endwalls for Culvert Pipe Steel 24-Inch	EACH	1.000		1.000	
0098	521.1030	Apron Endwalls for Culvert Pipe Steel 30-Inch	EACH	1.000		1.000	

Estimate Of Quantities

Line	Item	Item Description	Unit	4075-35-71		4075-35-72	
				Total	Qty	Qty	Qty
0100	521.1235	Apron Endwalls for Pipe Arch Steel 35x24-Inch	EACH	1.000	1.000		
0102	521.3124	Culvert Pipe Corrugated Steel 24-Inch	LF	18.000		18.000	
0104	521.3735	Pipe Arch Corrugated Steel 35x24-Inch	LF	10.000	10.000		
0106	601.0411	Concrete Curb & Gutter 30-Inch Type D	LF	916.000	6.000	910.000	
0108	601.0557	Concrete Curb & Gutter 6-Inch Sloped 36-Inch Type D	LF	130.000	51.000	79.000	
0110	601.0600	Concrete Curb Pedestrian	LF	35.000		35.000	
0112	602.0405	Concrete Sidewalk 4-Inch	SF	2,825.000	239.000	2,586.000	
0114	602.0415	Concrete Sidewalk 6-Inch	SF	910.000	48.000	862.000	
0116	602.0515	Curb Ramp Detectable Warning Field Natural Patina	SF	330.000	20.000	310.000	
0118	602.0615	Curb Ramp Detectable Warning Field Radial Natural Patina	SF	53.000		53.000	
0120	606.0200	Riprap Medium	CY	25.000	5.000	20.000	
0122	606.0300	Riprap Heavy	CY	170.000		170.000	
0124	608.0424	Storm Sewer Pipe Reinforced Concrete Class IV 24-Inch	LF	143.000		143.000	
0126	608.3012	Storm Sewer Pipe Class III-A 12-Inch	LF	25.000		25.000	
0128	608.3015	Storm Sewer Pipe Class III-A 15-Inch	LF	230.000		230.000	
0130	611.2003	Manholes 3-FT Diameter	EACH	1.000		1.000	
0132	611.8115	Adjusting Inlet Covers	EACH	1.000		1.000	
0134	611.9705	Salvaged Manhole Covers	EACH	1.000		1.000	
0136	612.0208	Pipe Underdrain Unperforated 8-Inch	LF	20.000		20.000	
0138	618.0100	Maintenance And Repair of Haul Roads (project) 01. 4075-35-71	EACH	1.000	1.000		
0140	618.0100	Maintenance And Repair of Haul Roads (project) 02. 4075-35-72	EACH	1.000		1.000	
0142	619.1000	Mobilization	EACH	1.000	0.250	0.750	
0144	624.0100	Water	MGAL	58.500	17.500	41.000	
0146	625.0100	Topsoil	SY	2,550.000	270.000	2,280.000	
0148	628.1504	Silt Fence	LF	500.000	90.000	410.000	
0150	628.1520	Silt Fence Maintenance	LF	1,000.000	180.000	820.000	
0152	628.1905	Mobilizations Erosion Control	EACH	5.000	2.000	3.000	
0154	628.1910	Mobilizations Emergency Erosion Control	EACH	4.000	2.000	2.000	
0156	628.2004	Erosion Mat Class I Type B	SY	1,700.000	200.000	1,500.000	
0158	628.2008	Erosion Mat Urban Class I Type B	SY	860.000	70.000	790.000	
0160	628.7005	Inlet Protection Type A	EACH	2.000		2.000	
0162	628.7015	Inlet Protection Type C	EACH	19.000		19.000	
0164	628.7555	Culvert Pipe Checks	EACH	46.000	20.000	26.000	
0166	628.7570	Rock Bags	EACH	58.000	21.000	37.000	
0168	629.0210	Fertilizer Type B	CWT	4.000	1.000	3.000	
0170	630.0130	Seeding Mixture No. 30	LB	33.000	4.000	29.000	
0172	630.0140	Seeding Mixture No. 40	LB	22.000	2.000	20.000	
0174	630.0500	Seed Water	MGAL	70.000	9.000	61.000	
0176	633.5200	Markers Culvert End	EACH	22.000	10.000	12.000	
0178	634.0614	Posts Wood 4x6-Inch X 14-FT	EACH	4.000		4.000	
0180	638.2102	Moving Signs Type II	EACH	4.000		4.000	
0182	638.3000	Removing Small Sign Supports	EACH	4.000		4.000	
0184	642.5001	Field Office Type B	EACH	1.000	0.250	0.750	
0186	643.0300	Traffic Control Drums	DAY	4,700.000	800.000	3,900.000	
0188	643.0410	Traffic Control Barricades Type II	DAY	180.000	25.000	155.000	
0190	643.0420	Traffic Control Barricades Type III	DAY	3,000.000	1,300.000	1,700.000	
0192	643.0705	Traffic Control Warning Lights Type A	DAY	4,800.000	2,200.000	2,600.000	
0194	643.0900	Traffic Control Signs	DAY	13,000.000	7,200.000	5,800.000	
0196	643.0920	Traffic Control Covering Signs Type II	EACH	5.000	3.000	2.000	

Estimate Of Quantities

Line	Item	Item Description	Unit	4075-35-71		4075-35-72	
				Total	Qty	Total	Qty
0198	643.1000	Traffic Control Signs Fixed Message	SF	26.000		26.000	
0200	643.1050	Traffic Control Signs PCMS	DAY	56.000	21.000	35.000	
0202	643.3165	Temporary Marking Line Paint 6-Inch	LF	40,900.000	20,300.000	20,600.000	
0204	643.5000	Traffic Control	EACH	1.000	0.250	0.750	
0206	644.1440	Temporary Pedestrian Surface Matting	SF	40.000		40.000	
0208	644.1601	Temporary Pedestrian Curb Ramp	DAY	10.000		10.000	
0210	644.1605	Temporary Pedestrian Detectable Warning Field	SF	10.000		10.000	
0212	644.1810	Temporary Pedestrian Barricade	LF	660.000		660.000	
0214	645.0120	Geotextile Type HR	SY	370.000	25.000	345.000	
0216	646.2020	Marking Line Epoxy 6-Inch	LF	12,700.000	4,700.000	8,000.000	
0218	646.2040	Marking Line Grooved Wet Ref Epoxy 6-Inch	LF	59,300.000	26,600.000	32,700.000	
0220	646.4040	Marking Line Grooved Wet Ref Epoxy 10-Inch	LF	1,230.000	780.000	450.000	
0222	646.7420	Marking Crosswalk Epoxy Transverse Line 6-Inch	LF	1,544.000	78.000	1,466.000	
0224	650.4000	Construction Staking Storm Sewer	EACH	4.000		4.000	
0226	650.5500	Construction Staking Curb Gutter and Curb & Gutter	LF	1,081.000	57.000	1,024.000	
0228	650.6000	Construction Staking Pipe Culverts	EACH	6.000	2.000	4.000	
0230	650.8000	Construction Staking Resurfacing Reference	LF	25,821.000	9,554.000	16,267.000	
0232	650.9000	Construction Staking Curb Ramps	EACH	27.000	2.000	25.000	
0234	650.9500	Construction Staking Sidewalk (project) 01. 4075-35-71	EACH	1.000	1.000		
0236	650.9500	Construction Staking Sidewalk (project) 02. 4075-35-72	EACH	1.000		1.000	
0238	650.9911	Construction Staking Supplemental Control (project) 01. 4075-35-71	EACH	1.000	1.000		
0240	650.9911	Construction Staking Supplemental Control (project) 02. 4075-35-72	EACH	1.000		1.000	
0242	690.0150	Sawing Asphalt	LF	2,170.000	181.000	1,989.000	
0244	690.0250	Sawing Concrete	LF	325.000	22.000	303.000	
0246	740.0440	Incentive IRI Ride	DOL	19,560.000	7,236.000	12,324.000	
0248	ASP.1T0A	On-the-Job Training Apprentice at \$5.00/HR	HRS	2,000.000	2,000.000		
0250	ASP.1T0G	On-the-Job Training Graduate at \$5.00/HR	HRS	1,320.000	1,320.000		
0252	SPV.0035	Special 01. Foundation Backfill	CY	620.000	131.000	489.000	
0254	SPV.0060	Special 01. Storm Sewer Tap	EACH	1.000		1.000	
0256	SPV.0090	Special 01. Cleaning Storm Sewer	LF	835.000		835.000	
0258	SPV.0180	Special 01. Removing Asphaltic Surface within Gutter	SY	70.000		70.000	

GRUBBING

CATEGORY	STATION	TO	STATION	LOCATION	201.0205	201.0220	REMARKS
					GRUBBING	GRUBBING	
					STA	ID	
PROJECT 4075-35-72							
0010	135+50	-	136+10	STH 96, RT	1	-	
0010	163+40	-	164+20	STH 96, RT	1	-	
0010	216+50	-	217+30	STH 96, RT	1	-	
0010			283+80	STH 96, RT	-	12	
PROJECT TOTALS					3	12	
TOTALS 0010					3	12	

REMOVING SMALL PIPE CULVERTS

CATEGORY	STATION	LOCATION	203.0100	REMARKS
			REMOVING SMALL PIPE CULVERTS EACH	
PROJECT 4075-35-71				
0010	52+00	STH 96	1	35"X24" ELIP CMCP , PARTIAL REMOVAL
0010	118+88	STH 96	1	36"X 70' RCCP
PROJECT TOTAL			2	
PROJECT 4075-35-72				
0010	164+15	STH 96	1	30" X 120' RCCP
0010	178+35	STH 96	1	30" X 100' RCCP
0010	217+54	STH 96	1	30" X 135' RCCP
0010	251+20	STH 96	1	24" CMCP, PARTIAL REMOVAL
PROJECT TOTAL			4	
TOTAL 0010			6	

REMOVING BUTT JOINTS

CATEGORY	STATION	TO	STATION	LOCATION	204.0115	REMARKS
					REMOVING ASPHALTIC SURFACE BUTT JOINTS SY	
PROJECT 4075-35-72						
0010			33+50	STH 96	8	BOP
0010				FOX MEDOWS LN	7	SIDE ROAD
0010				KELSO RD	6	SIDE ROAD
0010				RIVER FOREST DR	7	SIDE ROAD
0010	33+52	-	44+35	STH 96	38	PRIVATE ENTRANCES
0010	44+35	-	59+70	STH 96	35	PRIVATE ENTRANCES
0010	59+70	-	75+05	STH 96	11	PRIVATE ENTRANCES
0010	75+05	-	90+40	STH 96	32	PRIVATE ENTRANCES
0010	90+40	-	105+75	STH 96	9	PRIVATE ENTRANCES
0010	105+75	-	121+10	STH 96	56	PRIVATE ENTRANCES
0010	121+10	-	129+04	STH 96	74	PRIVATE ENTRANCES
PROJECT TOTAL					283	
PROJECT 4075-35-72						
0010	129+04			STH 96	200	PAVEMENT TRANSITION
0010				RIVERBEND DR	71	SIDE ROAD
0010				CTH JJ	62	SIDE ROAD
0010				SHAWN CT	65	SIDE ROAD
0010				DEERING LN	55	SIDE ROAD
0010				CTH U & LOCK RD	138	SIDE ROADS
0010	253+25			STH 96	200	PAVEMENT TRANSITION
0010				VAN DYKE ST & 3RD ST	15	SIDE ROADS
0010				NANCY ST	8	SIDE ROAD
0010				LINDA ST LT & RT	15	SIDE ROADS
0010				2ND ST	4	SIDE ROAD
0010				1ST ST	6	SIDE ROAD
0010				OAK ST	5	SIDE ROAD
0010				POPLAR ST	7	SIDE ROAD
0010				GREEN ST	7	SIDE ROAD
0010	129+04	-	136+45	STH 96	44	PRIVATE ENTRANCES
0010	151+80	-	167+15	STH 96	101	PRIVATE ENTRANCES
0010	167+15	-	182+50	STH 96	92	PRIVATE ENTRANCES
0010	213+20	-	228+55	STH 96	272	PRIVATE ENTRANCES
PROJECT TOTAL					1,367	
TOTAL 0010					1,650	

REMOVING ASPHALTIC SURFACES

REMOVING ASPHALTIC SURFACES CONTINUED

CATEGORY	STATION	TO	STATION	LOCATION	204.0110 REMOVING ASPHALTIC SURFACE SY	204.0120 REMOVING ASPHALTIC SURFACE MILLING SY	REMARKS
PROJECT 4075-35-71							
0010	33+50	-	68+47	STH 96, RT	-	5,828	
0010	33+52	-	43+65	STH 96, LT	-	1,688	
0010	43+60	-	48+60	STH 96 LT, LAMPLIGHTER DR	-	621	
0010	43+65	-	48+65	STH 96, LT	-	833	
0010	48+60	-	60+50	STH 96, LT	-	1,983	
0010	60+50	-	65+15	STH 96, LT	-	775	
0010	60+50	-	65+15	STH 96, LT, FARMLAND RD	-	403	
0010	65+15	-	104+55	STH 96, LT	-	6,567	
0010	68+47	-	71+89	STH 96, RT	-	646	
0010	71+89	-	73+90	STH 96, RT	-	335	
0010	73+90	-	77+70	STH 96, RT	-	633	
0010	73+90	-	77+70	STH 96 RT, FOX MEDOWS LN	-	549	
0010	77+70	-	103+50	STH 96, RT	-	4,300	
0010	103+50	-	107+30	STH 96, RT	-	633	
0010	103+50	-	107+30	STH 96 RT, RIVER FOREST DR	-	599	
0010	104+55	-	108+45	STH 96, LT	-	650	
0010	104+55	-	108+45	STH 96, LT, KELSO RD	-	687	
0010	107+30	-	129+04	STH 96, RT	-	3,623	
0010	108+45	-	129+04	STH 96, LT	-	3,432	
0010	44+82	-	44+71	STH 96, LT	6	-	LAMPLIGHTER DR SW QUAD
0010	45+25	-	45+36	STH 96, LT	8	-	LAMPLIGHTER DR NW QUAD
0010	51+90	-	52+10	STH 96, LT	34	-	CULVERT REPLACEMENT
0010	118+35	-	119+42	STH 96	357	-	CULVERT REPLACEMENT
PROJECT TOTALS					405	34,785	
PROJECT 4075-35-72							
0010	129+04	-	130+20	STH 96, RT	-	219	
0010	129+04	-	130+20	RIVER BEND DR	-	278	
0010	129+04	-	133+65	STH 96, LT	-	871	
0010	129+04	-	133+65	CTH JJ	-	693	
0010	130+20	-	136+33	STH 96, RT	-	1,158	
0010	133+65	-	225+25	STH 96, LT	-	17,302	
0010	136+33	-	139+99	STH 96, RT	-	691	
0010	136+99	-	157+05	STH 96, RT	-	3,789	
0010	157+05	-	158+85	STH 96, RT	-	340	
0010	157+05	-	158+85	SHAWN CT	-	308	
0010	158+85	-	225+55	STH 96, RT	-	12,599	
0010	212+00	-	212+40	DEERING LN	-	22	
0010	225+25	-	230+15	STH 96, LT	-	926	
0010	225+25	-	230+15	CTH U	-	679	
0010	225+55	-	227+10	STH 96, RT	-	293	
0010	225+55	-	227+10	LOCK RD	-	287	
0010	227+10	-	236+63	STH 96, RT	-	1,800	
0010	230+15	-	253+25	STH 96, LT	-	4,363	
0010	236+63	-	239+84	STH 96, RT	-	606	
0010	239+84	-	253+00	STH 96, RT	-	2,486	
PROJECT TOTALS					0	49,710	

CATEGORY	STATION	TO	STATION	LOCATION	204.0110 REMOVING ASPHALTIC SURFACE SY	204.0120 REMOVING ASPHALTIC SURFACE MILLING SY	REMARKS
PROJECT 4075-35-72							
0010	253+00	-	291+71	STH 96, RT	-	8,602	
0010	253+00	-	254+35	3RD ST	-	156	
0010	253+25	-	291+71	STH 96, LT	-	8,547	
0010	253+25	-	254+40	VAN DYKE ST	-	253	
0010	262+35	-	263+25	NANCY ST	-	144	
0010	271+10	-	271+95	LINDA ST, RT	-	160	
0010	271+10	-	271+95	LINDA ST, LT	-	143	
0010	274+55	-	275+00	2ND ST	-	51	
0010	277+55	-	276+20	1ST ST	-	76	
0010	283+80	-	284+30	OAK ST	-	79	
0010	285+10	-	286+05	POPLAR ST	-	225	
0010	289+90	-	290+50	GREEN ST	-	69	
0010	254+40	-	254+53	STH 96, RT	8	-	VAN DYKE NE QUAD
0010	254+32	-	254+47	STH 96, LT	10	-	VAN DYKE SE QUAD
0010	262+42	-	262+65	STH 96, LT	8	-	NANCY ST NW QUAD
0010	262+37	-	262+62	STH 96, RT	11	-	NANCY ST SW QUAD
0010	263+07	-	263+36	STH 96, LT	6	-	NANCY ST SE QUAD
0010	263+11	-	263+32	STH 96, RT	6	-	NANCY ST NE QUAD
0010	271+09	-	271+31	STH 96, RT	11	-	LINDA ST SW QUAD
0010	271+12	-	271+33	STH 96, LT	9	-	LINDA ST NW QUAD
0010	271+78	-	271+99	STH 96, LT	10	-	LINDA ST NE QUAD
0010	271+79	-	272+00	STH 96, RT	10	-	LINDA ST SE QUAD
0010	274+49	-	274+65	STH 96, RT	7	-	2ND ST SW QUAD
0010	274+82	-	275+06	STH 96, LT	9	-	2ND ST NE QUAD
0010	274+86	-	275+21	STH 96, RT	6	-	2ND ST SE QUAD
0010	277+46	-	277+68	STH 96, RT	10	-	1ST ST SW QUAD
0010	277+99	-	278+29	STH 96, RT	11	-	1ST ST SE QUAD
0010	278+04	-	278+30	STH 96, LT	8	-	1ST ST NE QUAD
0010	283+73	-	283+90	STH 96, LT	8	-	OAK ST SW QUAD
0010	284+20	-	284+41	STH 96, RT	8	-	OAK ST SE QUAD
0010	283+77	-	284+02	STH 96, RT	8	-	OAK ST NW QUAD
0010	284+86	-	285+29	STH 96, LT	8	-	POPLAR ST NW QUAD
0010	285+15	-	285+34	STH 96, RT	9	-	POPLAR ST SW QUAD
0010	285+85	-	286+15	STH 96, LT	5	-	POPLAR ST NE QUAD
0010	289+79	-	290+07	STH 96, RT	10	-	GREEN ST SW QUAD
0010	289+81	-	290+02	STH 96, LT	8	-	GREEN ST NW QUAD
0010	290+37	-	290+60	STH 96, RT	7	-	GREEN ST SE QUAD
0010	163+49	-	164+66	STH 96	416	-	CULVERT REPLACEMENT
0010	177+76	-	178+95	STH 96	424	-	CULVERT REPLACEMENT
0010	216+56	-	218+52	STH 96	697	-	CULVERT REPLACEMENT
0010	251+02	-	251+22	STH 96, RT	36	-	CULVERT REPLACEMENT
0010	260+70	-	262+49	STH 96	239	-	STORM SEWER REPLACEMENT
0010	271+40	-	273+80	STH 96, LT	256	-	STORM SEWER REPLACEMENT
0010	283+73	-	283+85	STH 96, LT	16	-	STORM SEWER REPLACEMENT
PROJECT SUBTOTALS					2,295	18,505	
PROJECT TOTALS					2,295	68,215	
TOTALS 0010					2,700	103,000	

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REMOVING CONCRETE SURFACES

CATEGORY	STATION	TO	STATION	LOCATION	204.0150	204.0155	REMARKS
					REMOVING CURB & GUTTER LF	REMOVING CONCRETE SIDEWALK SY	
PROJECT 4075-35-71							
0010	44+82	-	44+71	STH 96, LT	22	12	LAMPLIGHTER DR SW QUAD
0010	45+25	-	45+36	STH 96, LT	31	16	LAMPLIGHTER DR NW QUAD
PROJECT TOTALS					53	28	
PROJECT 4075-35-72							
0010	254+32	-	254+47	STH 96, LT	37	11	VAN DYKE ST NE QUAD
0010	254+40	-	254+53	STH 96, RT	29	13	VAN DYKE ST SE QUAD
0010	262+42	-	262+65	STH 96, LT	39	12	NANCY ST NW QUAD
0010	263+07	-	263+36	STH 96, LT	24	11	NANCY ST NE QUAD
0010	262+37	-	262+62	STH 96, RT	22	14	NANCY ST SW QUAD
0010	263+11	-	263+32	STH 96, RT	21	17	NANCY ST SE QUAD
0010	271+09	-	271+31	STH 96, RT	37	13	LINDA ST SW QUAD
0010	271+12	-	271+33	STH 96, LT	38	12	LINDA ST NW QUAD
0010	271+78	-	271+99	STH 96, LT	30	11	LINDA ST NE QUAD
0010	271+79	-	272+00	STH 96, RT	35	11	LINDA ST SE QUAD
0010	271+97	-	273+77	STH 96, LT	180	0	FOR STORM SEWER REPLACEMENT
0010	274+49	-	274+65	STH 96, RT	21	10	2ND ST SW QUAD
0010	274+86	-	275+21	STH 96, RT	30	18	2ND ST SE QUAD
0010	274+82	-	275+06	STH 96, LT	22	17	2ND ST NE QUAD
0010	277+46	-	277+68	STH 96, RT	36	12	1ST ST SW QUAD
0010	277+99	-	278+29	STH 96, RT	42	14	1ST ST SE QUAD
0010	278+04	-	278+30	STH 96, LT	32	18	1ST ST NE QUAD
0010	283+73	-	283+90	STH 96, LT	30	14	OAK ST NW QUAD
0010	283+77	-	284+02	STH 96, RT	27	18	OAK ST SW QUAD
0010	284+20	-	284+41	STH 96, RT	24	12	OAK ST SE QUAD
0010	284+86	-	285+29	STH 96, LT	25	30	POPLAR ST NW QUAD
0010	285+85	-	286+15	STH 96, LT	29	13	POPLAR ST NE QUAD
0010	285+15	-	285+34	STH 96, RT	15	12	POPLAR ST SW QUAD
0010	289+79	-	290+14	STH 96, RT	33	21	GREEN ST SW QUAD
0010	290+37	-	290+60	STH 96, RT	24	12	GREEN ST SE QUAD
0010	289+81	-	290+02	STH 96, LT	25	21	GREEN ST NE QUAD
PROJECT TOTALS					907	367	
TOTALS 0010					960	395	

REMOVING ASPHALTIC SURFACE WITHIN GUTTER

CATEGORY	STATION	TO	STATION	LOCATION	SPV.0180.01
					REMOVING ASPHALTIC SURFACE WITHIN GUTTER SY
PROJECT 4075-35-72					
0010	136+33	-	139+45	STH 96	70
PROJECT TOTAL					70
TOTAL 0010					70

REMOVING STORM SEWER

CATEGORY	STATION	TO	STATION	LOCATION	204.0245.01	204.0245.02	204.0245.04	204.0245.05	204.0270
					REMOVING 12-INCH STORM SEWER LF	REMOVING 15-INCH STORM SEWER LF	REMOVING 24-INCH STORM SEWER LF	REMOVING 30-INCH STORM SEWER LF	ABANDONING CULVERT PIPES EACH
PROJECT 4075-35-72									
0010	135+40	-	136+39	STH 96, RT	-	-	-	-	1
0010	163+89	-	164+09	STH 96, RT	-	-	-	56	-
0010	260+75	-	262+18	STH 96, RT	-	-	143	-	-
0010	262+51	-	262+71	STH 96, LT	21	-	-	-	-
0010	271+44	-	273+74	STH 96, LT	-	230	-	-	-
0010	283+75	-	183+79	STH 96, LT	-	4	-	-	-
PROJECT TOTALS					21	234	143	56	1
TOTALS 0010					21	234	143	56	1

REMOVING MANHOLES, INLETS AND APRON ENDWALLS

CATEGORY	STATION	LOCATION	204.0240	204.0220	204.9060.S.01
			REMOVING MANHOLES EACH	REMOVING INLETS EACH	REMOVING APRON ENDWALLS EACH
PROJECT 4075-35-72					
0010	135+40	STH 96, RT	-	-	1
0010	136+39	STH 96, RT	-	1	-
0010	164+10	STH 96, RT	-	1	-
0010	234+62	STH 96, RT	-	-	1
0010	283+79	STH 96, LT	1	-	-
PROJECT TOTALS			1	2	2
TOTALS 0010			1	2	2

BASE AGGREGATE

BASE AGGREGATE CONTINUED

CATEGORY	STATION	TO	STATION	LOCATION	305.0110	305.0120	624.0100	REMARKS
					BASE	BASE		
					AGGREGATE	AGGREGATE		
					DENSE	DENSE	WATER	
	3/4-INCH	1 1/4-INCH	MGAL					
PROJECT 4075-35-71								
0010	33+52	-	68+47	STH 96, RT	210	-	2.1	MAINLINE
0010	33+52	-	44+50	STH 96, LT	66	-	0.7	MAINLINE
0010	33+52	-	129+04	STH 96	52	-	0.7	PRIVATE ENTRANCES
0010	33+52	-	129+04	STH 96	24	-	0.3	SIDEROAD SHOULDERS
0010	44+79	-	44+87	STH 96, LT	-	4	-	LAMPLIGHTER DR SW QUAD
0010	45+17	-	45+40	STH 96, LT	-	6	-	LAMPLIGHTER DR NW QUAD
0010	45+75	-	60+80	STH 96, LT	91	-	1.0	MAINLINE
0010	51+90	-	52+10	STH 96, LT	-	35	0.4	CULVERT REPLACEMENT
0010	62+20	-	105+25	STH 96, LT	259	-	2.6	MAINLINE
0010	71+89	-	75+85	STH 96, RT	24	-	0.3	MAINLINE
0010	77+10	-	105+30	STH 96, RT	170	-	1.7	MAINLINE
0010	106+60	-	128+80	STH 96, RT	134	-	1.4	MAINLINE
0010	106+60	-	129+04	STH 96, LT	135	-	1.4	MAINLINE
0010	118+35	-	119+42	STH 96	-	488	4.9	CULVERT REPLACEMENT
PROJECT TOTALS					1,165	533	17.5	

CATEGORY	STATION	TO	STATION	LOCATION	305.0110	305.0120	624.0100	REMARKS
					BASE	BASE		
					AGGREGATE	AGGREGATE		
					DENSE	DENSE	WATER	
	3/4-INCH	1 1/4-INCH	MGAL					
PROJECT 4075-35-72								
0010	129+90	-	136+33	STH 96, RT	45	-	0.5	MAINLINE
0010	130+40	-	136+33	STH 96, LT	41	-	0.5	MAINLINE
0010	136+33	-	139+99	STH 96, LT	22	-	0.3	MAINLINE
0010	139+99	-	157+25	STH 96, RT	119	-	1.2	MAINLINE
0010	139+99	-	225+50	STH 96, LT	513	-	5.2	MAINLINE
0010	158+55	-	211+95	STH 96, RT	366	-	3.7	MAINLINE
0010	212+55	-	215+75	STH 96, RT	22	-	0.3	MAINLINE
0010	216+50	-	225+60	STH 96, RT	63	-	0.7	MAINLINE
0010	227+00	-	236+63	STH 96, RT	66	-	0.7	MAINLINE
0010	227+35	-	253+25	STH 96, LT	156	-	1.6	MAINLINE
0010	239+84	-	253+50	STH 96, RT	94	-	1.0	MAINLINE
0010	254+24	-	254+62	STH 96, LT	-	5	-	VAN DYKE ST NE QUAD
0010	254+16	-	254+46	STH 96, RT	-	5	-	VAN DYKE ST SE QUAD
0010	262+39	-	262+74	STH 96, LT	-	6	-	NANCY ST NW QUAD
0010	263+05	-	263+34	STH 96, LT	-	5	-	NANCY ST NE QUAD
0010	262+35	-	262+61	STH 96, RT	-	5	-	NANCY ST SW QUAD
0010	263+07	-	263+32	STH 96, RT	-	5	-	NANCY ST SE QUAD
0010	271+11	-	271+42	STH 96, LT	-	6	-	LINDA ST NW QUAD
0010	271+70	-	271+99	STH 96, LT	-	5	-	LINDA ST NE QUAD
0010	271+09	-	271+40	STH 96, RT	-	6	-	LINDA ST SW QUAD
0010	271+69	-	272+00	STH 96, RT	-	5	-	LINDA ST SE QUAD
0010	274+55	-	274+70	STH 96, RT	-	4	-	2ND ST SW QUAD
0010	274+78	-	275+06	STH 96, RT	-	6	-	2ND ST SE QUAD
0010	274+80	-	275+06	STH 96, LT	-	5	-	2ND ST NE QUAD
0010	277+42	-	277+63	STH 96, RT	-	5	-	1ST ST SW QUAD
0010	277+96	-	278+36	STH 96, RT	-	6	-	1ST ST SE QUAD
0010	278+98	-	278+33	STH 96, LT	-	6	-	1ST ST NE QUAD
0010	283+77	-	283+96	STH 96, RT	-	5	-	OAK ST SW QUAD
0010	284+12	-	284+34	STH 96, RT	-	4	-	OAK ST SE QUAD
0010	283+70	-	284+03	STH 96, LT	-	5	-	OAK ST NW QUAD
0010	285+09	-	285+35	STH 96, LT	-	8	-	POPLAR ST NW QUAD
0010	285+75	-	286+05	STH 96, LT	-	5	-	POPLAR ST NE QUAD
0010	285+15	-	285+34	STH 96, RT	-	4	-	POPLAR ST SE QUAD
0010	289+79	-	290+08	STH 96, RT	-	7	-	GREEN ST SW QUAD
0010	290+31	-	390+53	STH 96, RT	-	4	-	GREEN ST SE QUAD
0010	289+87	-	290+16	STH 96, LT	-	6	-	GREEN ST NW QUAD
0010	135+50	-	136+45	STH 96, RT	-	10	-	CURB & GUTTER
0010	129+04	-	253+25	STH 96	16	-	0.3	PRIVATE ENTRANCES
0010	129+04	-	253+25	STH 96	12	-	0.2	SIDEROAD SHOULDERS
0010	163+49	-	164+66	STH 96	-	534	5.4	CULVERT REPLACEMENT
0010	177+76	-	178+95	STH 96	-	543	5.5	CULVERT REPLACEMENT
0010	216+56	-	218+52	STH 96	-	894	9.0	CULVERT REPLACEMENT
0010	251+02	-	251+22	STH 96	-	44	0.5	CULVERT REPLACEMENT
0010	260+70	-	262+49	STH 96	-	172	1.8	STORM SEWER REPLACEMENT
0010	271+40	-	273+80	STH 96, LT	-	231	2.4	STORM SEWER REPLACEMENT
0010	283+69	-	283+85	STH 96, LT	-	16	0.2	STORM SEWER REPLACEMENT
PROJECT TOTALS					1,535	2,577	41.0	
TOTALS 0010					2,700	3,110	58.5	

EXCAVATION ITEMS

CATEGORY	STATION	TO	STATION	LOCATION	205.0100	208.0100	SPV.0035.01	REMARKS
					EXCAVATION	BORROW	FOUNDATION	
					COMMON		BACKFILL	
					CY	CY	CY	
PROJECT 4075-35-71								
0010	118+35	-	119+42	STH 96	280	-	131	CULVERT TRANSITIONS
0010	44+71	-	45+36	STH 96, LT	15	-	-	SIDEWALK AND CURB
PROJECT TOTALS					295	0	131	
PROJECT 4075-35-72								
0010	135+50	-	136+45	STH 96, RT	28	-	-	CURB & GUTTER
0010	163+49	-	164+66	STH 96	308	-	144	CULVERT TRANSITIONS
0010	177+76	-	178+95	STH 96	319	-	150	CULVERT TRANSITIONS
0010	216+56	-	218+52	STH 96	415	-	195	CULVERT TRANSITIONS
0010	254+26	-	290+14	STH 96	225	-	-	SIDEWALK AND CURB REPLACEMENTS
TOTALS 0010					1,295	0	489	
PROJECT 4075-35-72								
0030	163+50	-	164+50	STH 96, RT	40	80	-	CULVERT DITCH STA 164+00
TOTALS 0030					40	80	0	
PROJECT TOTALS					1,335	80	489	
4075-35-71/72 TOTALS 0010					1,590	0	620	
4075-35-71/72 TOTALS 0030					40	80	0	

HMA PAVEMENT

						211.0400	455.0605	460.6223	460.6224	465.0105				
						PREPARE								
						FOUNDATION								
						FOR ASPHALTIC			HMA PAVEMENT	HMA PAVEMENT	ASPHALTIC			
						SHOULDERS	TACK COAT	3 MT 58-28 S	4 MT 58-28 S	SURFACE				
CATEGORY	STATION	TO	STATION	LOCATION	STA	GAL	TON	TON	TON	TON	REMARKS			
PROJECT 4075-35-71														
0010	33+52	-	68+47	STH 96, RT	35	373	-	715	34		MAINLINE TYPICAL			
0010	33+52	-	43+65	STH 96, LT	11	109	-	208	10		MAINLINE TYPICAL			
0010	43+60	-	48+60	STH 96 LT, LAMPLIGHTER DR	-	38	-	72	-		SIDEROAD AND ADDT'L TURN LANE			
0010	43+65	-	48+65	STH 96, LT	-	54	-	103	-		MAINLINE TYPICAL			
0010	48+60	-	60+50	STH 96, LT	12	127	-	244	12		MAINLINE TYPICAL			
0010	60+50	-	65+15	STH 96, LT	-	50	-	96	-		MAINLINE TYPICAL			
0010	60+50	-	65+15	STH 96, LT, FARMLAND RD	-	25	-	47	-		SIDEROAD AND ADDT'L TURN LANE			
0010	65+15	-	104+55	STH 96, LT	40	421	-	806	76		MAINLINE TYPICAL			
0010	68+47	-	71+89	STH 96, RT	-	39	-	75	-		MAINLINE WITH CURB & GUTTER			
0010	71+89	-	73+90	STH 96, RT	3	22	-	42	2		MAINLINE TYPICAL			
0010	73+90	-	77+70	STH 96, RT	-	41	-	78	-		MAINLINE TYPICAL			
0010	73+90	-	77+70	STH 96 RT, FOX MEDOWS LN	-	33	-	64	-		SIDEROAD AND ADDT'L TURN LANE			
0010	77+70	-	103+50	STH 96, RT	26	276	-	528	25		MAINLINE TYPICAL			
0010	103+50	-	107+30	STH 96, RT	-	41	-	78	-		MAINLINE TYPICAL			
0010	103+50	-	107+30	STH 96 RT, RIVER FOREST DR	-	36	-	69	-		SIDEROAD AND ADDT'L TURN LANE			
0010	104+55	-	108+45	STH 96, LT	-	42	-	80	-		MAINLINE TYPICAL			
0010	104+55	-	108+45	STH 96, LT, KELSO RD	-	42	-	79	-		SIDEROAD AND ADDT'L TURN LANE			
0010	107+30	-	129+04	STH 96, RT	22	232	-	445	21		MAINLINE TYPICAL			
0010	108+45	-	129+04	STH 96, LT	21	220	-	421	40		MAINLINE TYPICAL			
0010	51+90	-	52+10	STH 96, LT	-	5	-	-	13		CULVERT REPLACEMENT			
0010	118+35	-	119+42	STH 96	-	46	-	-	137		CULVERT REPLACEMENT			
PROJECT TOTALS					170	2,272	0	4,250	370					

ITEMS CONTINUED ON NEXT SHEET

HMA PAVEMENT CONTINUED

CATEGORY	STATION	TO	STATION	LOCATION	211.0400	455.0605	460.6223	460.6224	465.0105	REMARKS
					PREPARE FOUNDATION FOR ASPHALTIC SHOULDERS	TACK COAT	HMA PAVEMENT 3 MT 58-28 S	HMA PAVEMENT 4 MT 58-28 S	ASPHALTIC SURFACE	
STATION	TO	STATION	LOCATION	STA	GAL	TON	TON	TON		
PROJECT 4075-35-72										
0010	129+04	-	130+20	STH 96, RT	-	27	29	23	-	MAINLINE TYPICAL
0010	129+04	-	130+20	RIVER BEND DR	-	42	45	35	-	SIDEROAD AND TAPER
0010	129+04	-	133+65	STH 96, LT	-	105	113	88	-	MAINLINE TYPICAL
0010	129+04	-	133+65	CTH JJ	-	91	98	76	-	SIDEROAD AND ADDT'L TURN LANE
0010	130+20	-	136+33	STH 96, RT	-	139	150	117	-	MAINLINE TYPICAL
0010	133+65	-	225+25	STH 96, LT	-	2,077	2,239	1,742	-	MAINLINE TYPICAL
0010	136+33	-	139+99	STH 96, RT	-	83	90	70	-	MAINLINE WITH CURB & GUTTER
0010	136+99	-	157+05	STH 96, RT	-	455	491	382	-	MAINLINE TYPICAL
0010	157+05	-	158+85	STH 96, RT	-	41	44	35	-	MAINLINE TYPICAL
0010	157+05	-	158+85	SHAWN CT	-	45	49	38	-	SIDEROAD AND TAPER
0010	158+85	-	225+55	STH 96, RT	-	1,512	1,630	1,268	-	MAINLINE TYPICAL
0010	212+00	-	212+40	DEERING LN	-	10	10	8	-	SIDEROAD APPROACH
0010	225+55	-	227+10	STH 96, RT	-	36	38	30	-	MAINLINE TYPICAL
0010	225+55	-	227+10	LOCK RD	-	42	46	36	-	SIDEROAD AND TAPER
0010	225+25	-	230+15	STH 96, LT	-	112	120	94	-	MAINLINE TYPICAL
0010	225+25	-	230+15	CTH U	-	84	94	73	-	SIDEROAD AND ADDT'L TURN LANE
0010	227+10	-	236+63	STH 96, RT	-	217	233	182	-	MAINLINE TYPICAL
0010	230+15	-	253+25	STH 96, LT	-	524	565	440	-	MAINLINE TYPICAL
0010	236+63	-	239+84	STH 96, RT	-	73	79	69	-	MAINLINE WITH CURB & GUTTER OVERLAY
0010	239+84	-	253+00	STH 96, RT	-	299	322	251	-	MAINLINE TYPICAL
0010	253+00	-	291+71	STH 96, RT	-	388	-	758	-	MAINLINE WITH CURB & GUTTER OUTSIDE OF PARKING LANE
0010	253+00	-	254+35	3RD ST	-	10	-	18	-	SIDEROAD AND TAPER
0010	253+25	-	291+71	STH 96, LT	-	380	-	625	-	MAINLINE WITH CURB & GUTTER OUTSIDE OF PARKING LANE
0010	271+10	-	271+95	LINDA ST, RT	-	10	-	19	-	SIDEROAD APPROACH
0010	274+55	-	275+00	2ND ST	-	4	-	6	-	SIDEROAD APPROACH
0010	277+55	-	276+20	1ST ST	-	5	-	9	-	SIDEROAD APPROACH
0010	283+80	-	284+30	OAK ST	-	5	-	10	-	SIDEROAD APPROACH
0010	289+90	-	290+50	GREEN ST	-	5	-	8	-	SIDEROAD APPROACH
0010	253+25	-	254+40	VAN DYKE ST	-	16	-	30	-	SIDEROAD AND TAPER
0010	262+35	-	263+25	NANCY ST	-	9	-	17	-	SIDEROAD APPROACH
0010	271+10	-	271+95	LINDA ST, LT	-	9	-	17	-	SIDEROAD APPROACH
0010	285+10	-	286+05	POPLAR ST	-	14	-	26	-	SIDEROAD APPROACH
0010	163+49	-	164+66	STH 96	-	54	-	-	159	CULVERT REPLACEMENT
0010	177+76	-	178+95	STH 96	-	54	-	-	162	CULVERT REPLACEMENT
0010	216+56	-	218+52	STH 96	-	89	-	-	266	CULVERT REPLACEMENT
0010	251+02	-	251+22	STH 96, RT	-	5	-	-	14	CULVERT REPLACEMENT
0010	260+70	-	262+49	STH 96	-	29	-	-	55	STORM SEWER REPLACEMENT
0010	271+40	-	273+80	STH 96, LT	-	31	-	-	59	STORM SEWER REPLACEMENT
0010	283+69	-	283+85	STH 96, LT	-	3	-	-	5	STORM SEWER REPLACEMENT
TOTALS 0010					0	7,134	6,485	6,600	720	
PROJECT 4075-35-72										
0020	254+56	-	291+71	STH 96, RT	-	129	-	232	-	6-FT PARKING LANE OUTSIDE OF INTERSECTIONS AND CROSSWALKS
0020	254+60	-	291+71	STH 96, LT	-	133	-	236	-	6-FT PARKING LANE OUTSIDE OF INTERSECTIONS AND CROSSWALKS
0020	254+10	-	289+88	STH 96	-	-	-	-	4	PATCHES AT WATER VALVES
TOTALS 0020					0	262	0	468	4	
PROJECT TOTAL					0	7,396	6,485	7,068	724	
4075-35-71/72 TOTALS 0010					170	9,406	6,485	10,850	1,090	
4075-35-71/72 TOTALS 0020					0	262	0	468	4	

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ASPHALTIC SURFACE PATCHING

CATEGORY	STATION	TO	STATION	LOCATION	465.0110 ASPHALTIC SURFACE PATCHING TON	REMARKS
PROJECT 4075-35-71						
0010	44+79	-	44+87	STH 96, LT	2	LAMPLIGHTER DR SW QUAD
0010	45+17	-	45+40	STH 96, LT	3	LAMPLIGHTER DR NW QUAD
PROJECT TOTAL					5	
PROJECT 4075-35-72						
0010	254+24	-	254+62	STH 96, LT	3	VAN DYKE ST NE QUAD
0010	254+16	-	254+46	STH 96, RT	3	VAN DYKE ST SE QUAD
0010	262+39	-	262+74	STH 96, LT	4	NANCY ST NW QUAD
0010	263+05	-	263+34	STH 96, LT	3	NANCY ST NE QUAD
0010	262+35	-	262+61	STH 96, RT	2	NANCY ST SW QUAD
0010	263+07	-	263+32	STH 96, RT	2	NANCY ST SE QUAD
0010	271+11	-	271+42	STH 96, LT	3	LINDA ST NW QUAD
0010	271+70	-	271+99	STH 96, LT	3	LINDA ST NE QUAD
0010	271+09	-	271+40	STH 96, RT	3	LINDA ST SW QUAD
0010	271+69	-	272+00	STH 96, RT	3	LINDA ST SE QUAD
0010	274+55	-	274+70	STH 96, RT	2	2ND ST SW QUAD
0010	274+78	-	275+06	STH 96, RT	3	2ND ST SE QUAD
0010	274+80	-	275+06	STH 96, LT	2	2ND ST NE QUAD
0010	277+42	-	277+63	STH 96, RT	3	1ST ST SW QUAD
0010	277+96	-	278+36	STH 96, RT	4	1ST ST SE QUAD
0010	278+98	-	278+33	STH 96, LT	3	1ST ST NE QUAD
0010	283+77	-	283+96	STH 96, RT	3	OAK ST SW QUAD
0010	284+12	-	284+34	STH 96, RT	2	OAK ST SE QUAD
0010	283+70	-	284+03	STH 96, LT	3	OAK ST NW QUAD
0010	285+09	-	285+35	STH 96, LT	3	POPLAR ST NW QUAD
0010	285+75	-	286+05	STH 96, LT	3	POPLAR ST NE QUAD
0010	285+15	-	285+34	STH 96, RT	2	POPLAR ST SE QUAD
0010	289+79	-	290+08	STH 96, RT	3	GREEN ST SW QUAD
0010	290+31	-	390+53	STH 96, RT	3	GREEN ST SE QUAD
0010	289+87	-	290+16	STH 96, LT	2	GREEN ST NW QUAD
PROJECT TOTAL					70	
TOTAL 0010					75	

RUMBLE STRIPS

CATEGORY	STATION	TO	STATION	LOCATION	465.0520 ASPHALTIC RUMBLE STRIPS, SHOULDER LF	465.0560 ASPHALTIC RUMBLE STRIPS, CENTERLINE LF	REMARKS
PROJECT 4075-35-71							
0010	64+30	-	74+50	STH 96	-	1,020	START EAST OF FARMLAND DR
0010	78+50	-	104+00	STH 96	-	2,550	
0010	108+00	-	125+80	STH 96	-	1,780	
0010	64+30	-	68+20	STH 96, RT	390	-	
0010	72+35	-	73+35	STH 96, RT	100	-	
0010	78+25	-	102+75	STH 96, RT	2,000	-	
0010	108+00	-	126+75	STH 96, RT	1,605	-	
0010	65+80	-	103+80	STH 96, LT	3,260	-	
0010	110+00	-	125+95	STH 96, LT	1,505	-	
PROJECT TOTALS					8,860	5,350	
PROJECT 4075-35-72							
0010	131+50	-	155+90	STH 96	-	2,440	
0010	159+90	-	210+20	STH 96	-	5,030	
0010	214+20	-	224+50	STH 96	-	1,030	
0010	228+50	-	252+00	STH 96	-	2,350	
0010	130+85	-	156+15	STH 96, RT	2,350	-	
0010	159+50	-	210+90	STH 96, RT	4,420	-	
0010	213+00	-	224+50	STH 96, RT	970	-	
0010	228+00	-	252+20	STH 96, RT	2,060	-	
0010	134+20	-	219+40	STH 96, LT	8,430	-	
0010	230+85	-	251+75	STH 96, LT	2,090	-	
PROJECT TOTALS					20,320	10,850	
TOTALS 0010					29,180	16,200	

PWL MIXTURE USE TABLE

THE FOLLOWING ACCEPTANCE CRITERIA ARE APPLICABLE FOR THIS PROJECT:

LOCATION	STATION TO STATION	MIXTURE USE	UNDERLYING SURFACE	BID ITEM	TONS	DEPTH	QUALITY MANAGEMENT PROGRAM TO BE USED FOR:	
							MIXTURE ACCEPTANCE	DENSITY ACCEPTANCE
11-FT DRIVING LANES	33+50 - 129+04	UPPER LAYER	MILLED EXISTING HMA SURFACE	4 MT 58-28S	2,686	2"	PWL INCENTIVE AIR VOIDS HMA PAVEMENT 460.2010	INCENTIVE DENSITY PWL HMA PAVEMENT 460.2005
PAVED SHOULDER AND SIDEROADS	33+50 - 129+04	UPPER LAYER	MILLED EXISTING HMA SURFACE	4 MT 58-28S	1,564	2"	PWL INCENTIVE AIR VOIDS HMA PAVEMENT 460.2010	ACCEPTANCE TESTING BY DEPARTMENT, NOT ELIGIBLE FOR INCENTIVE OR DISINCENTIVE
VARIOUS	33+50 - 129+04	CULVERT PATCHES AND SHOULDER WIDENING	ASPHLATIC BASE AGGREGATE SURFACE	370		6 1/4" TOTAL	QMP AS PER SS 460	ACCEPTANCE BY ORDINARY COMPACTION
12-FT DRIVING LANES	129+04 - 253+25	UPPER LAYER	3 MT 58-28S	4 MT 58-28S	3,333	1 3/4"	PWL INCENTIVE AIR VOIDS HMA PAVEMENT 460.2010	INCENTIVE DENSITY PWL HMA PAVEMENT 460.2005
PAVED SHOULDER AND SIDEROADS	129+04 - 253+25	UPPER LAYER	3 MT 58-28S	4 MT 58-28S	1,724	1 3/4"	PWL INCENTIVE AIR VOIDS HMA PAVEMENT 460.2010	ACCEPTANCE TESTING BY DEPARTMENT, NOT ELIGIBLE FOR INCENTIVE OR DISINCENTIVE
12-FT DRIVING LANES	129+04 - 253+25	LOWER LAYER	MILLED EXISTING HMA SURFACE	3 MT 58-28S	4,286	2 1/4"	QMP AS PER SS 460	INCENTIVE DENSITY HMA PAVEMENT 460.2000
PAVED SHOULDER AND SIDEROADS	129+04 - 253+25	LOWER LAYER	MILLED EXISTING HMA SURFACE	3 MT 58-28S	2,199	2 1/4"	QMP AS PER SS 460	ACCEPTANCE TESTING BY DEPARTMENT, NOT ELIGIBLE FOR INCENTIVE OR DISINCENTIVE
VARIOUS	129+04 - 253+25	CULVERT PATCHES	ASPHLATIC BASE AGGREGATE SURFACE	602		6 1/4" TOTAL	QMP AS PER SS 460	ACCEPTANCE BY ORDINARY COMPACTION
12-FT DRIVING LANES	253+25 - 291+71	UPPER LAYER	MILLED EXISTING HMA SURFACE	4 MT 58-28S	1,180	2"	PWL INCENTIVE AIR VOIDS HMA PAVEMENT 460.2010	INCENTIVE DENSITY PWL HMA PAVEMENT 460.2005
URBAN SIDEROADS, PAVED SHOULDER AND PARKING LANES	253+25 - 291+71	UPPER LAYER	MILLED EXISTING HMA SURFACE	4 MT 58-28S	831	2"	PWL INCENTIVE AIR VOIDS HMA PAVEMENT 460.2010	ACCEPTANCE TESTING BY DEPARTMENT, NOT ELIGIBLE FOR INCENTIVE OR DISINCENTIVE
VARIOUS	253+25 - 291+71	STORM SEWER REPAIRS	ASPHLATIC BASE AGGREGATE SURFACE	118		4" TOTAL	QMP AS PER SS 460	ACCEPTANCE BY ORDINARY COMPACTION

ASPHALTIC SURFACE DRIVEWAYS

465.0120
ASPHALTIC SURFACE
DRIVEWAYS AND FIELD
ENTRANCES

CATEGORY	STATION TO STATION	LOCATION	TON
PROJECT 4075-35-71			
0010	33+52 - 44+35	STH 96	9
0010	44+35 - 59+70	STH 96	8
0010	59+70 - 75+05	STH 96	3
0010	75+05 - 90+40	STH 96	8
0010	90+40 - 105+75	STH 96	2
0010	105+75 - 121+10	STH 96	13
0010	121+10 - 129+04	STH 96	17
PROJECT TOTAL			60
PROJECT 4075-35-72			
0010	129+04 - 136+45	STH 96	3
0010	151+80 - 167+15	STH 96	6
0010	167+15 - 182+50	STH 96	5
0010	213+20 - 228+55	STH 96	16
PROJECT TOTAL			30
TOTAL 0010			90

ASPHALTIC FLUMES

465.0315
ASPHALTIC
FLUMES

CATEGORY	STATION	LOCATION	SY
PROJECT 4075-35-72			
0010	135+60	STH 96, RT	5
PROJECT TOTAL			5
TOTAL 0010			5

3

CULVERT PIPE

CATEGORY	STATION	LOCATION	520.3430	520.3436	520.8700	521.3124	521.3735	REQUIRED STEEL THICKNESS INCHES
			CULVERT PIPE CLASS III-A NON-METAL 30-INCH LF	CULVERT PIPE CLASS III-A NON-METAL 36-INCH LF	CLEANING CULVERT PIPES EACH	CULVERT PIPE CORRUGATED STEEL 24-INCH LF	PIPE ARCH CORRUGATED STEEL 35X24-INCH LF	
PROJECT 4075-35-71								
0010	39+78	STH 96	-	-	1	-	-	-
0010	52+00	STH 96, LT	-	-	-	-	10	0.064
0010	118+88	STH 96	-	60	-	-	-	-
PROJECT TOTALS			0	60	1	0	10	
PROJECT 4075-35-72								
0010	164+08	STH 96	-	106	-	-	-	-
0010	178+36	STH 96	96	-	-	-	-	-
0010	217+54	STH 96	138	-	-	-	-	-
0010	251+20	STH 96, RT	-	-	-	18	-	0.064
PROJECT TOTALS			234	106	0	18	0	
TOTALS 0010			234	166	1	18	10	

APRON ENDWALLS AND CULVERT MARKERS

CATEGORY	STATION	LOCATION	520.1030	520.1036	521.1024	521.1030	521.1235	633.5200	REMARKS
			APRON ENDWALLS FOR CULVERT PIPE 30-INCH EACH	APRON ENDWALLS FOR CULVERT PIPE 36-INCH EACH	APRON ENDWALLS FOR CULVERT PIPE STEEL 24-INCH EACH	APRON ENDWALLS FOR CULVERT PIPE STEEL 30-INCH EACH	APRON ENDWALLS FOR PIPE ARCH STEEL 35X24-INCH EACH	MARKERS CULVERT END EACH	
PROJECT 4075-35-71									
0010	39+78	STH 96	-	-	-	-	-	2	MARKERS ONLY
0010	52+00	STH 96	-	-	-	-	1	2	END WALL LEFT
0010	75+84	STH 96	-	-	-	-	-	2	MARKERS ONLY
0010	113+46	STH 96	-	-	-	-	-	2	MARKERS ONLY
0010	118+88	STH 96	-	2	-	-	-	2	
PROJECT TOTALS			0	2	0	0	1	10	
PROJECT 4075-35-72									
0010	132+27	STH 96	-	-	-	-	-	2	MARKERS ONLY
0010	164+08	STH 96	-	2	-	-	-	2	
0010	178+36	STH 96	2	-	-	-	-	2	
0010	217+54	STH 96	2	-	-	-	-	2	
0010	234+62	STH 96	-	-	-	1	-	2	END WALL RIGHT
0010	251+04	STH 96	-	-	1	-	-	2	END WALL RIGHT
PROJECT TOTALS			4	2	1	1	0	12	
TOTALS 0010			4	4	1	1	1	22	

CLEANING STORM SEWER

CATEGORY	STATION TO	STATION	LOCATION	SPV.0090.01 CLEANING STORM SEWER	REMARKS
				LF	
PROJECT 4075-35-72					
0010	262+18	- 264+77	STH 96	258	
0010	264+77	- 264+84	STH 96, RT	22	
0010	264+77	- 264+94	STH 96, LT	27	
0010	271+07	- 271+23	STH 96, LT	18	
0010	271+38	- 271+44	STH 96, LT	33	
0010	273+74	- 274+42	STH 96, LT	68	
0010		277+06	STH 96, LT	11	
0010	280+22	- 283+75	STH 96, LT	352	
0010	289+75	- 289+88	STH 96, LT	46	
PROJECT TOTAL				835	
TOTAL 0010				835	

UNDERDRAIN

CATEGORY	STATION TO	STATION	LOCATION	612.0208 PIPE UNDERDRAIN UNPERFORATED 8-INCH
				LF
PROJECT 4075-35-72				
0010	163+80	- 163+90	STH 96, RT	20
PROJECT TOTAL				20
TOTAL 0010				20

3

STORM SEWER

CATEGORY	STATION TO	STATION	LOCATION	CONCRETE COLLARS FOR PIPE EACH	STORM SEWER PIPE REINFORCED CONCRETE CLASS IV 24-INCH LF	STORM SEWER PIPE CLASS III-A 12-INCH LF	STORM SEWER PIPE CLASS III-A 15-INCH LF	MANHOLES 3-FT DIAMETER EACH	ADJUSTING INLET COVERS EACH	SALVAGING MANHOLE COVERS EACH	STORM SEWER TAP EACH
PROJECT 4075-35-72											
0010	260+75	- 262+18	STH 96	-	143	-	-	-	-	-	1
0010	262+51	- 262+71	STH 96, LT	-	-	21	-	-	-	-	-
0010	271+44	- 273+74	STH 96, LT	-	-	-	230	-	1	-	-
0010	283+79		STH 96	1	-	4	-	1	-	1	-
PROJECT TOTALS				1	143	25	230	1	1	1	1
TOTALS 0010				1	143	25	230	1	1	1	1

RIPRAP

CATEGORY	STATION TO	STATION	LOCATION	606.0200 RIPRAP MEDIUM CY	606.0300 RIPRAP HEAVY CY	645.0120 GEOTEXTILE TYPE HR SY
PROJECT 4075-35-71						
0010	118+35	- 119+40	STH 96	4	-	23
0010	UNDISTRIBUTED			1	-	2
PROJECT TOTALS				5	0	25
PROJECT 4075-35-72						
0010	177+78	- 178+93	STH 96	5	-	25
0010	216+80	- 218+27	STH 96	6	-	33
0010	284+83	- 234+97	STH 96	5	-	25
0010	UNDISTRIBUTED			4	-	22
TOTALS 0010				20	0	105
PROJECT 4075-35-72						
0030	163+50	- 164+50	STH 96, RT	-	170	240
TOTALS 0030				0	170	240
PROJECT TOTALS				20	170	345
4075-35-71/72 TOTALS 0010				25	0	130
4075-35-71/72 TOTALS 0030				0	170	240

INLET PROTECTION

CATEGORY	STATION TO	STATION	LOCATION	628.7005 INLET PROTECTION TYPE A EACH	628.7015 INLET PROTECTION TYPE C EACH
PROJECT 4075-35-72					
0010	262+30	- 263+15	STH 96, NANCYST	1	3
0010	271+20	- 272+10	STH 96, LINDAST	1	4
0010	275+50	- 274+65	STH 96, 2ND ST	-	3
0010	277+60	- 278+00	STH 96, 1ST ST	-	3
0010	285+00	- 285+77	STH 96, POPLAR ST	-	4
0010	289+70	- 289+90	STH 96, GREEN ST	-	2
PROJECT TOTALS				2	19
TOTALS 0010				2	19

CURB & GUTTER ITEMS

CATEGORY	STATION	TO	STATION	LOCATION	416.0610 DRILLED TIE BARS EACH	601.0411 CONCRETE CURB & GUTTER 30-INCH TYPE D LF	601.0557 CONCRETE CURB & GUTTER 6-INCH SLOPED 36-INCH TYPE D LF	601.0600 CONCRETE CURB PEDESTRIAN LF	REMARKS
PROJECT 4075-35-71									
0010	44+82	-	44+71	STH 96, LT	8	2	22	-	LAMPLIGHTER DR NW QUAD
0010	45+25	-	45+36	STH 96, LT	8	4	29	-	LAMPLIGHTER DR NE QUAD
PROJECT TOTALS					16	6	51	0	
PROJECT 4075-35-72									
0010	135+65	-	136+44	STH 96, LT	4	-	79	-	
0010	254+26	-	254+60	STH 96, LT	4	37	-	-	VAN DYKE ST NE QUAD
0010	254+19	-	254+45	STH 96, RT	4	29	-	-	VAN DYKE ST SE QUAD
0010	262+41	-	262+72	STH 96, LT	4	39	-	-	NANCY ST NW QUAD
0010	262+37	-	262+61	STH 96, RT	4	22	-	-	NANCY ST SW QUAD
0010	263+07	-	263+32	STH 96, LT	4	27	-	-	NANCY ST NE QUAD
0010	263+09	-	263+30	STH 96, RT	4	21	-	-	NANCY ST SE QUAD
0010	271+12	-	271+41	STH 96, LT	4	38	-	-	LINDA ST NW QUAD
0010	271+10	-	271+38	STH 96, RT	4	37	-	-	LINDA ST SW QUAD
0010	271+73	-	271+97	STH 96, LT	4	30	-	-	LINDA ST NE QUAD
0010	271+71	-	271+98	STH 96, RT	4	35	-	-	LINDA ST SE QUAD
0010	271+97	-	273+77	STH 96, RT	4	180	-	-	FOR STORM SEWER REPLACEMENT
0010	274+57	-	274+67	STH 96, RT	4	21	-	-	2ND ST SW QUAD
0010	274+82	-	275+02	STH 96, RT	4	30	-	-	2ND ST SE QUAD
0010	274+82	-	275+05	STH 96,LT	4	22	-	-	2ND ST NE QUAD
0010	277+44	-	277+75	STH 96, RT	4	36	-	-	1ST ST SW QUAD
0010	277+98	-	278+35	STH 96, RT	4	42	-	-	1ST ST SE QUAD
0010	278+00	-	278+32	STH 96, LT	4	32	-	-	1ST ST NE QUAD
0010	283+77	-	283+94	STH 96, RT	4	27	-	-	OAK ST SW QUAD
0010	284+15	-	284+32	STH 96,RT	4	24	-	-	OAK ST SE QUAD
0010	283+72	-	284+02	STH 96, LT	4	30	-	-	OAK ST NW QUAD
0010	285+12	-	285+32	STH 96, LT	4	25	-	-	POPLAR ST NW QUAD
0010	285+78	-	286+04	STH 96, LT	4	29	-	-	POPLAR ST NE QUAD
0010	285+17	-	285+32	STH 96, RT	4	15	-	-	POPLAR ST SW QUAD
0010	289+80	-	290+05	STH 96, RT	4	33	-	-	GREEN ST SW QUAD
0010	290+33	-	290+51	STH 96, RT	4	24	-	-	GREEN ST SE QUAD
0010	289+89	-	290+14	STH 96, LT	4	25	-	35	GREEN ST NW QUAD
PROJECT TOTALS					108	910	79	35	
TOTALS 0010					124	916	130	35	

CONCRETE SIDEWALK

CATEGORY	STATION	TO	STATION	LOCATION	602.0405	602.0415	602.0515	602.0615	REMARKS
					CONCRETE SIDEWALK 4-INCH SF	CONCRETE SIDEWALK 6-INCH SF	CURB RAMP DETECTABLE WARNING FIELD NATURAL PATINA SF	CURB RAMP DETECTABLE WARNING FIELD RADIAL NATURAL PATINA SF	
PROJECT 4075-35-71									
0010	44+71	-	44+82	STH 96, LT	80	27	10	-	LAMPLIGHTER DR SW QUAD
0010	45+25	-	45+36	STH 96, LT	159	21	10	-	LAMPLIGHTER DR NW QUAD
PROJECT TOTALS					239	48	20	0	
PROJECT 4075-35-72									
0010	254+40	-	254+53	STH 96, LT	62	16	10	-	VAN DYKE NE QUAD
0010	254+32	-	254+47	STH 96, RT	70	21	10	-	VAN DYKE SE QUAD
0010	262+42	-	262+65	STH 96, LT	57	79	20	-	NANCY ST NW QUAD
0010	262+36	-	262+62	STH 96, RT	150	-	10	-	NANCY ST SW QUAD
0010	263+06	-	263+37	STH 96, RT	174	-	10	-	NANCY ST SE QUAD
0010	263+11	-	263+32	STH 96, LT	65	49	20	-	NANCY ST NE QUAD
0010	271+08	-	271+31	STH 96, RT	74	49	20	-	LINDA ST SW QUAD
0010	271+11	-	271+33	STH 96, LT	58	69	20	-	LINDA ST NW QUAD
0010	271+78	-	272+00	STH 96, LT	66	49	20	-	LINDA ST NE QUAD
0010	271+78	-	272+00	STH 96, RT	58	67	20	-	LINDA ST SE QUAD
0010	274+48	-	274+65	STH 96, RT	48	33	10	-	2ND ST SW QUAD
0010	274+82	-	275+07	STH 96, LT	148	-	10	-	2ND ST NE QUAD
0010	274+85	-	275+21	STH 96, RT	115	46	20	-	2ND ST SE QUAD
0010	277+46	-	277+69	STH 96, RT	68	33	-	17	1ST ST SW QUAD
0010	278+03	-	278+30	STH 96, RT	61	54	10	17	1ST ST SE QUAD
0010	277+98	-	278+30	STH 96, LT	163	-	10	-	1ST ST NE QUAD
0010	283+73	-	283+93	STH 96, RT	146	39	10	-	OAK ST SW QUAD
0010	284+20	-	284+42	STH 96, RT	79	48	10	-	OAK ST SE QUAD
0010	283+76	-	284+02	STH 96, LT	125	-	10	-	OAK ST NW QUAD
0010	284+86	-	285+30	STH 96, LT	243	58	10	-	POPLAR ST NW QUAD
0010	285+15	-	285+34	STH 96, RT	104	-	10	-	POPLAR ST SW QUAD
0010	285+85	-	286+15	STH 96, LT	95	37	-	19	POPLAR ST NE QUAD
0010	289+81	-	290+02	STH 96, RT	112	73	20	-	GREEN ST SW QUAD
0010	290+37	-	290+60	STH 96, RT	64	42	10	-	GREEN ST SE QUAD
0010	289+79	-	290+14	STH 96, LT	181	-	10	-	GREEN ST NW QUAD
PROJECT TOTALS					2,586	862	310	53	
TOTALS 0010					2,825	910	330	53	

EROSION CONTROL MOBILIZATIONS

CATEGORY	LOCATION	628.1905	628.1910	REMARKS
		MOBILIZATIONS EROSION CONTROL EACH	MOBILIZATIONS EMERGENCY EROSION CONTROL EACH	
PROJECT 4075-35-71				
0010		2	2	
PROJECT TOTALS				
PROJECT 4075-35-72				
0010		3	2	
PROJECT TOTALS				
TOTALS 0010		5	4	

SILT FENCE

CATEGORY	STATION	TO	STATION	LOCATION	628.1504	628.1520
					SILT FENCE LF	SILT FENCE MAINTENANCE LF
PROJECT 4075-35-71						
0010	118+35	-	119+40	STH 96	90	180
PROJECT TOTALS					90	180
PROJECT 4075-35-72						
0010	177+78	-	178+93	STH 96	67	134
0010	216+80	-	218+27	STH 96	215	430
0010	234+83	-	234+97	STH 96	43	86
0010	251+02	-	251+22	STH 96	39	78
0010	262+06	-	263+67	STH 96	46	92
PROJECT TOTALS					410	820
TOTALS 0010					500	1,000

LANDSCAPING

CATEGORY	STATION	TO	STATION	LOCATION	625.0100	628.2004	628.2008	629.0210	630.0130	630.0140	630.0500	REMARKS
					TOPSOIL SY	EROSION MAT CLASS I TYPE B SY	EROSION MAT URBAN CLASS I TYPE B SY	FERTILIZER TYPE B CWT	SEEDING MIXTURE NO. 30 LB	SEEDING MIXTURE NO. 40 LB	SEED WATER MGAL	
PROJECT 4075-35-71												
0010	44+29	-	45+86	STH 95	55	-	55	0.2	-	1.5	2	LAMPLIGHTER DR
0010	51+90	-	52+10	STH 96	19	19	-	0.1	0.4	-	1	CULVERT
0010	118+35	-	119+40	STH 96	139	139	-	0.2	2.5	-	4	CULVERT
				UNDISTRIBUTED	57	42	15	0.5	1.1	0.5	2	
PROJECT TOTALS					270	200	70	1.0	4.0	2.0	9	
PROJECT 4075-35-72												
0010	135+35	-	135+45	STH 96, RT	8	8	-	0.1	0.2	-	1	ENDWALL REMOVAL
0010	135+50	-	136+45	STH 96, RT	80	-	80	0.1	1.5	-	2	CURB & GUTTER
0010	163+50	-	164+65	STH 96	670	670	-	0.5	12.1	-	16	CULVERT
0010	177+78	-	178+93	STH 96	238	238	-	0.2	4.3	-	6	CULVERT
0010	216+80	-	218+27	STH 96	242	242	-	0.2	4.4	-	6	CULVERT
0010	234+83	-	234+97	STH 96	8	8	-	0.1	0.2	-	1	CULVERT
0010	251+02	-	251+22	STH 96	25	25	-	0.1	0.5	-	1	CULVERT
0010	253+43	-	255+01	STH 96	44	-	44	0.1	-	1.2	1	VAN DYKE ST
0010	262+06	-	263+67	STH 96	88	-	88	0.1	-	2.4	2	NANCY ST
0010	270+70	-	272+00	STH 96	64	-	64	0.1	-	1.8	2	LINDA ST
0010	272+00	-	273+80	STH 96	60	-	60	0.1	-	1.7	2	CURB FOR STORM SEWER REPLACEMENT
0010	274+03	-	275+63	STH 96	59	-	59	0.1	-	1.6	2	2ND STREET
0010	277+09	-	278+66	STH 96	87	-	87	0.1	-	2.4	2	1ST STREET
0010	283+23	-	284+75	STH 96	34	-	34	0.1	-	1.0	1	OAK ST
0010	284+80	-	286+34	STH 96	56	-	56	0.1	-	1.6	2	POPLAR ST
0010	289+30	-	290+90	STH 96	56	-	56	0.1	-	1.6	2	GREEN ST
				UNDISTRIBUTED	461	309	162	0.8	5.8	4.7	12	
PROJECT TOTALS					2,280	1,500	790	3.0	29.0	20.0	61	
TOTALS 0010					2,550	1,700	860	4.0	33.0	22.0	70	

PIPE CHECKS

CATEGORY	STATION	TO	STATION	LOCATION	628.7555	628.7570
					CULVERT PIPE CHECKS EACH	ROCK BAGS EACH
PROJECT 4075-35-71						
0010	51+90	-	52+10	STH 96	10	14
0010	118+35	-	119+40	STH 96	10	7
PROJECT TOTALS					20	21
PROJECT 4075-35-72						
0010	163+50	-	164+65	STH 96	10	14
0010	177+78	-	178+93	STH 96	8	10
0010	216+80	-	218+27	STH 96	8	10
0010	251+02	-	251+22	STH 96	-	3
PROJECT TOTALS					26	37
TOTALS 0010					46	58

MOVING SIGNS

CATEGORY	STATION	LOCATION	634.0614	638.2102	638.3000	REMARKS
			POSTS WOOD 4X6-INCH X 14-FT EACH	MOVING SIGNS TYPE II EACH	REMOVING SMALL SIGN SUPPORTS EACH	
PROJECT 4075-35-72						
0010	262+52	STH 96, RT	1	1	1	EXISTING SIGN IN PROPOSED SIDEWALK
0010	262+65	STH 96, LT	1	1	1	DISTURBED BY STORM SEWER WORK
0010	271+35	STH 96, LT	1	1	1	DISTURBED BY CURB & GUTTER WORK
0010	290+05	STH 96, LT	1	1	1	DISTURBED BY SIDEWALK WORK
PROJECT TOTALS			4	4	4	
TOTALS 0010			4	4	4	

TRAFFIC CONTROL

LOCATION	APPROXIMATE SERVICE PERIOD	643.0300		643.0410		643.0420		643.0705		643.0900		643.1050		REMARKS
		NO. IN SERVICE	TRAFFIC CONTROL DRUMS DAY	NO. IN SERVICE	TRAFFIC CONTROL BARRICADES TYPE II DAY	NO. IN SERVICE	TRAFFIC CONTROL BARRICADES TYPE III DAY	NO. IN SERVICE	TRAFFIC CONTROL WARNING LIGHTS TYPE A DAY	NO. IN SERVICE	TRAFFIC CONTROL SIGNS DAY	NO. IN SERVICE	TRAFFIC CONTROL SIGNS PCMS DAY	
PROJECT 4075-35-71														
STAGE 1 - CULVERT REPLACEMENT, MILLING AND PAVING SOUTH OF CTH U														
MAINLINE ADVANCED WARNING	49	0	0	0	0	8	392	16	784	6	294	1	7	MAINLINE ADVANCED SIGNING WHEN STH 96 IS CLOSED AND DETOURED
SIDEROAD CLOSURES	49	0	0	0	0	10	490	20	980	10	490	0	0	BARRICADES AND SIGNS FOR SIDEROAD CLOSURES DETAIL 4
MAINLINE CLOSURES FOR CULVERT WORK	3	0	0	0	0	10	30	12	36	6	18	0	0	BARRICADES AND SIGNS FOR MAINLINE CLOSURES DETAIL D
CULVERT AREAS AFTER REPLACEMENT PRIOR TO PAVING	10	6	60	0	0	0	0	0	0	4	40	0	0	LOOSE GRAVEL WARINING SITUATIONS
MILLING AND PAVING	40	16	640	0	0	4	160	0	0	8	320	0	0	MILLED SURFACE AND UNEVEN LANE SITUATIONS
SIDEWALK CLOSURE LAMPLIGHTER DR	5	0	0	4	20	0	0	0	0	4	20	0	0	SEE TRAFFIC CONTROL PEDESTRIAN ACCOMODATION
SIDEWALK AND CURB RAMPS WORKING	5	10	50	0	0	0	0	0	0	4	20	0	0	WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY
SIDEWALK AND CURB RAMPS PROTECTING	5	4	20	0	0	0	0	0	0	0	0	0	0	PROTECT PAVEMENT DROPOFFS NEAR CURBS BEFORE SURFACE PAVING
UNDISTRIBUTED			30		5		81		106		69		0	
PROJECT TOTALS			800		25		1,153		1,906		1,271		7	
PROJECT 4075-35-72														
STAGE 1 - CULVERT REPLACEMENT, MILLING AND PAVING SOUTH OF CTH U														
MAINLINE ADVANCED WARNING	49	0	0	0	0	0	0	0	0	6	294	1	7	MAINLINE ADVANCED SIGNING WHEN STH 96 IS CLOSED AND DETOURED
SIDEROAD CLOSURES	49	0	0	0	0	10	490	20	980	10	490	0	0	BARRICADES AND SIGNS FOR SIDEROAD CLOSURES DETAIL 4
MAINLINE CLOSURES FOR CULVERT WORK	10	0	0	0	0	10	100	12	120	6	60	0	0	BARRICADES AND SIGNS FOR MAINLINE CLOSURES DETAIL D
CULVERT AREAS AFTER REPLACEMENT PRIOR TO PAVING	15	10	150	0	0	0	0	0	0	6	90	0	0	LOOSE GRAVEL WARINING SITUATIONS
MILLING AND PAVING	40	16	640	0	0	4	160	0	0	8	320	0	0	MILLED SURFACE AND UNEVEN LANE SITUATIONS
CURB & GUTTER STA 135+50 TO 136+45	10	10	100	0	0	0	0	0	0	2	20	0	0	PROTECT WORK ZONE
STAGE 2 CURB RAMP WORK - (CONCURRENT WITH STAGE 1 WORK)														
CURB RAMPS STAGE 2A	7	40	280	4	28	0	0	0	0	24	168	0	0	SEE TRAFFIC CONTROL PEDESTRIAN ACCOMODATION
CURB RAMPS STAGE 2B	7	45	315	4	28	0	0	0	0	28	196	0	0	SEE TRAFFIC CONTROL PEDESTRIAN ACCOMODATION
CURB RAMPS STAGE 2C	7	55	385	4	28	0	0	0	0	32	224	0	0	SEE TRAFFIC CONTROL PEDESTRIAN ACCOMODATION
CURB RAMPS STAGE 2D	7	40	280	4	28	0	0	0	0	20	140	0	0	SEE TRAFFIC CONTROL PEDESTRIAN ACCOMODATION
CURB RAMPS STAGE 2E	7	16	112	2	14	0	0	0	0	17	119	0	0	SEE TRAFFIC CONTROL PEDESTRIAN ACCOMODATION
PATCHING PAVEMENT AROUND CURB RAMPS	10	50	500	0	0	0	0	0	0	0	0	0	0	PROTECT PAVEMENT DROPOFFS NEAR CURBS BEFORE SURFACE PAVING
STAGE 3 - STORM SEWER, MILLING AND PAVING NORTH OF CTH U														
CURB RAMP AND CURB & GUTTER STAGE 3A	7	4	28	2	14	0	0	0	0	10	70	0	0	SEE TRAFFIC CONTROL PEDESTRIAN ACCOMODATION
MAINLINE ADVANCED WARNING	31	0	0	0	0	0	0	0	0	10	310	2	14	MAINLINE ADVANCED SIGNING WHEN STH 96 IS CLOSED AND DETOURED
SIDEROAD CLOSURES	31	0	0	0	0	18	558	36	1,116	18	558	0	0	BARRICADES AND SIGNS FOR SIDEROAD CLOSURES DETAIL 4
MAINLINE AND SIDEROAD CLOSURES FOR STORM SEWER	10	20	200	0	0	10	100	12	120	6	60	0	0	BARRICADES AND SIGNS FOR MAINLINE CLOSURES DETAIL D
STORM SEWER WORK AREA AFTER REPLACEMENT PRIOR TO PAVING	10	20	200	0	0	0	0	0	0	8	80	0	0	LOOSE GRAVEL WARINING SITUATIONS
MILLING AND PAVING	25	16	400	0	0	4	100	0	0	8	200	0	0	MILLED SURFACE AND UNEVEN LANE SITUATIONS
MILLING AND PAVING CTH U INTERSECTION	3	20	60	0	0	10	30	0	0	6	18	0	0	
UNDISTRIBUTED			250		15		100		140		368		0	
PROJECT TOTALS			3,900		155		1,638		2,476		3,785		21	
TOTALS 0010			4,700		180		2,791		4,382		5,056		28	

*ADDITIONAL QUANTITIES SHOWN ELSEWHERE

TRAFFIC CONTROL DETOUR SIGN SUMMARY - PROJECT 4075-35-71

SIGN NO.	LOCATION	SIGN CODE	SIZE W X H	NUMBER IN SERVICE	APPROX. SERVICE PERIOD 49 DAYS	* 643.0900 SIGNS DAYS	* 643.0420 BARRICADES TYPE III DAYS	* 643.0705 WARNING LIGHTS TYPE A DAYS	* 643.1050 SIGNS PORTABLE CHANGEABLE MESSAGE DAYS	NO OF CYCLES	* 643.0920 COVERING SIGNS TYPE II EACH	REMARKS
1	STH 55/96, S. OF STH 55/96 SPLIT, PLACE 2000' S. OF STH 55/96 INTERSECTION	M 3-2	24"x12"	1	49	49						
	"	M 1-6	24"x24"	1	49	49						96
	"	W 20-2A	48"x48"	1	49	49						
2	STH 55/96, S. OF STH 55/96 SPLIT, PLACE 1000' S. OF STH 55/96 INTERSECTION	MO 4-8	24"x12"	1	49	49						
	"	M 3-2	24"x12"	1	49	49						
	"	M 1-6	24"x24"	1	49	49						96
	"	MO 5-1L	21"x21"	1	49	49						
3	STH 55/96, S. OF STH 55/96 SPLIT, COVER EXISTING D1-3 SIGN AS SHOWN									1	1	COVER "WRIGHTSTOWN TILT ARROW"
4	STH 55/96, AT STH 55/96 SPLIT, MODIFY EXISTING J3-2 SIGN AS SHOWN	MO 4-8	24"x12"	1	49	49						
	"	MO 6-1	21"x21"	1	49	49						LEFT
5	STH 55/96, AT STH 55/96 SPLIT, COVER EXISTING J3-4 SIGN AS SHOWN									1	1	COVER "EAST 96 AHEAD"
6	STH 96, AT STH 55/96, PLACE ON RIGHT SHOULDER, IN SE QUADRANT OF INTERSECTION	R 11-3	60"x30"	1	49	49	49	98				1 MILE AHEAD
7	STH 55, N. OF STH 55/96 SPLIT, PLACE 200' N. OF STH 55/96 INTERSECTION	MO 4-8	24"x12"	1	49	49						
	"	M 3-2	24"x12"	1	49	49						
	"	M 1-6	24"x24"	1	49	49						96
8	STH 55, N. OF STH 55/96 SPLIT, MODIFY EXISTING J1-2 SIGN AS SHOWN	M 4-8A	24"x18"	1	49	49						
9	STH 55, S. OF CTH 00, PLACE 150' S. OF CTH 00 INTERSECTION	MO 4-8	24"x12"	1	49	49						
	"	M 3-4	24"x12"	1	49	49						
	"	M 1-6	24"x24"	1	49	49						96
10	STH 55, S. OF CTH 00, PLACE 250' S. OF CTH 00 INTERSECTION	MO 4-8	24"x12"	1	49	49						
	"	M 3-2	24"x12"	1	49	49						
	"	M 1-6	24"x24"	1	49	49						96
	"	MO 6-1	21"x21"	1	49	49						AHEAD
11	STH 55, AT CTH 00, PLACE NEXT TO EXISTING SPLITTER ISLAND IN RAB INTERSECTION	MO 4-8	24"x12"	1	49	49						
	"	M 3-4	24"x12"	1	49	49						
	"	M 1-6	24"x24"	1	49	49						96
	"	MO 6-2	21"x21"	1	49	49						TILT RIGHT
12	STH 55, AT CTH 00, PLACE NEXT TO EXISTING SPLITTER ISLAND IN RAB INTERSECTION	MO 4-8	24"x12"	1	49	49						
	"	M 3-2	24"x12"	1	49	49						
	"	M 1-6	24"x24"	1	49	49						96
	"	MO 6-2	21"x21"	1	49	49						TILT RIGHT
13	STH 55, N. OF CTH 00, PLACE 250' N. OF CTH 00 INTERSECTION	MO 4-8	24"x12"	1	49	49						
	"	M 3-4	24"x12"	1	49	49						
	"	M 1-6	24"x24"	1	49	49						96
	"	MO 6-1	21"x21"	1	49	49						AHEAD
14	STH 55, S. OF MALONEY RD, PLACE 250' S. OF MALONEY RD INTERSECTION	MO 4-8	24"x12"	1	49	49						
	"	M 3-2	24"x12"	1	49	49						
	"	M 1-6	24"x24"	1	49	49						96
	"	MO 6-1	21"x21"	1	49	49						AHEAD
15	STH 55, N. OF MALONEY RD, PLACE 250' N. OF MALONEY RD INTERSECTION	MO 4-8	24"x12"	1	49	49						
	"	M 3-4	24"x12"	1	49	49						
	"	M 1-6	24"x24"	1	49	49						96
	"	MO 6-1	21"x21"	1	49	49						AHEAD
PAGE SUBTOTALS				41		2,009	49	98	0		2	

*ADDITIONAL QUANTITIES SHOWN ELSEWHERE

SHEET INFORMATION PROVIDED BY WISDOT

TRAFFIC CONTROL DETOUR SIGN SUMMARY - PROJECT 4075-35-71 CONTINUED

SIGN NO.	LOCATION	SIGN CODE	SIZE W X H	NUMBER IN SERVICE	APPROX. SERVICE PERIOD 49 DAYS	* 643.0900	* 643.0420	* 643.0705	* 643.1050	NO OF CYCLES	* 643.0920	REMARKS
						SIGNS	BARRICADES TYPE III	WARNING LIGHTS TYPE A	SIGNS PORTABLE CHANGEABLE MESSAGE		COVERING SIGNS TYPE II EACH	
16	STH 55, S. OF I-41 NB RAMP RAB, PLACE 150' S. OF I-41 NB RAMP INTERSECTION	MO 4-8	24"x12"	1	49	49						
	"	M 3-2	24"x12"	1	49	49						
	"	M 1-6	24"x24"	1	49	49						96
	"	MO 6-1	21"x21"	1	49	49						RIGHT
17	STH 55, AT I-41 NB, PLACE NEXT TO EXISTING SPLITTER ISLAND IN RAB INTERSECTION	MO 4-8	24"x12"	1	49	49						
	"	M 3-4	24"x12"	1	49	49						
	"	M 1-6	24"x24"	1	49	49						96
	"	MO 6-2	21"x21"	1	49	49						TILT RIGHT
18	STH 55, AT I-41 SB, PLACE NEXT TO EXISTING SPLITTER ISLAND IN RAB INTERSECTION	MO 4-8	24"x12"	1	49	49						
	"	M 3-4	24"x12"	1	49	49						
	"	M 1-6	24"x24"	1	49	49						96
	"	MO 6-2	21"x21"	1	49	49						TILT RIGHT
19	I-41 SB OFF-RAMP TO STH 55, PLACE 150' N. OF RAMP INTERSECTION	MO 4-8	24"x12"	1	49	49						
	"	M 3-4	24"x12"	1	49	49						
	"	M 1-6	24"x24"	1	49	49						96
	"	MO 6-1	21"x21"	1	49	49						LEFT
20	I-41, AT STH 55 EXIT, PLACE LEFT OF EXISTING GROUND MOUNT SIGN N. OF STH 55 EXIT RAMP	MO 4-8	36"x18"	1	49	49						
	"	M 3-4	36"x18"	1	49	49						
	"	M 1-6	36"x36"	1	49	49						96
	"	MO 6-2	30"x30"	1	49	49						TILT RIGHT
21	I-41, N. OF STH 55, PLACE RIGHT OF EXISTING J4-2 ON I-41 NB	MO 4-8	36"x18"	1	49	49						
	"	M 3-2	36"x18"	1	49	49						
	"	M 1-6	36"x36"	1	49	49						96
22	I-41, N. OF STH 55, PLACE 1/4 MILE N. OF STH 55 EXIT ON RIGHT SHOULDER	MO 4-8	36"x18"	1	49	49						
	"	M 3-4	36"x18"	1	49	49						
	"	M 1-6	36"x36"	1	49	49						96
	"	MO 5-2R	30"x30"	1	49	49						
23	I-41, N. OF STH 55, PLACE 1/4 MILE N. OF STH 55 EXIT IN MEDIAN	MO 4-8	36"x18"	1	49	49						
	"	M 3-4	36"x18"	1	49	49						
	"	M 1-6	36"x36"	1	49	49						96
	"	MO 5-2R	30"x30"	1	49	49						
24	I-41, S. OF CTH J, PLACE LEFT OF EXISTING GROUND MOUNT SIGN AT CTH J EXIT	MO 4-8	36"x18"	1	49	49						
	"	M 3-2	36"x18"	1	49	49						
	"	M 1-6	36"x36"	1	49	49						96
	"	MO 6-1	30"x30"	1	49	49						AHEAD
25	I-41, N. OF CTH J, PLACE LEFT OF EXISTING GROUND MOUNT SIGN AT CTH J EXIT	MO 4-8	36"x18"	1	49	49						
	"	M 3-4	36"x18"	1	49	49						
	"	M 1-6	36"x36"	1	49	49						96
	"	MO 6-1	30"x30"	1	49	49						AHEAD
26	CTH JJ, AT STH 96, COVER EXISTING J13-1 SIGN AS SHOWN									1	1	COVER ENTIRE SIGN
27	CTH JJ, AT STH 96, PLACE IN ROADWAY AT STH 96 INTERSECTION	R 11-4	60"x30"	1	49	49	49	98				
28	STH 96, PLACE ON RIGHT SHOULDER, FIELD DETERMINE LOCATION	PCMS		1					7			PLACE IN ADVANCE OF CLOSURE

PAGE SUBTOTALS

37

1,764

49

98

7

1

*ADDITIONAL QUANTITIES SHOWN ELSEWHERE

SHEET INFORMATION PROVIDED BY WISDOT

TRAFFIC CONTROL DETOUR SIGN SUMMARY - PROJECT 4075-35-71 CONTINUED

SIGN NO.	LOCATION	SIGN CODE	SIZE W X H	NUMBER IN SERVICE	APPROX. SERVICE PERIOD 49 DAYS	* 643.0900	* 643.0420	* 643.0705	* 643.1050	NO OF CYCLES	* 643.0920	REMARKS
						SIGNS	BARRICADES TYPE III	WARNING LIGHTS TYPE A	SIGNS PORTABLE CHANGEABLE MESSAGE		COVERING SIGNS TYPE II	
29	I-41, S. OF CTH U, PLACE 1/4 MILE S. OF CTH U IN MEDIAN	MO 4-8	36"x18"	1	49	49						
	"	M 3-2	36"x18"	1	49	49						
	"	M 1-6	36"x36"	1	49	49						96
	"	MO 5-2R	30"x30"	1	49	49						
30	I-41, S. OF CTH U, PLACE 1/4 MILE S. OF CTH U ON RIGHT SHOULDER	MO 4-8	36"x18"	1	49	49						
	"	M 3-2	36"x18"	1	49	49						
	"	M 1-6	36"x36"	1	49	49						96
	"	MO 5-2R	30"x30"	1	49	49						
31	I-41, S. OF CTH U ON-RAMP, PLACE RIGHT OF EXISTING J4-2 SIGN	MO 4-8	36"x18"	1	49	49						
	"	M 3-4	36"x18"	1	49	49						
	"	M 1-6	36"x36"	1	49	49						96
32	I-41, S. OF CTH U OFF-RAMP, PLACE LEFT OF EXISTING CANTILEVER SIGN	MO 4-8	36"x18"	1	49	49						
	"	M 3-2	36"x18"	1	49	49						
	"	M 1-6	36"x36"	1	49	49						96
	"	MO 6-2	30"x30"	1	49	49						TILT RIGHT
33	I-41 OFF-RAMP TO CTH U, PLACE 500' PRIOR TO CTH U RAMP INTERSECTION	MO 4-8	24"x12"	1	49	49						
	"	M 3-2	24"x12"	1	49	49						
	"	M 1-6	24"x24"	1	49	49						96
	"	MO 5-1R	21"x21"	1	49	49						
34	I-41 OFF-RAMP TO CTH U, PLACE RIGHT OF R1-1 AT CTH U RAMP INTERSECTION	MO 4-8	24"x12"	1	49	49						
	"	M 3-2	24"x12"	1	49	49						
	"	M 1-6	24"x24"	1	49	49						96
	"	MO 6-1	21"x21"	1	49	49						RIGHT
35	CTH U, BETWEEN I-41 RAMPS, PLACE LEFT OF EXISTING J2-2 SIGN	MO 4-8	24"x12"	1	49	49						
	"	M 3-4	24"x12"	1	49	49						
	"	M 1-6	24"x24"	1	49	49						96
	"	MO 5-1L	21"x21"	1	49	49						
36	CTH U, BETWEEN I-41 RAMPS, PLACE IN MEDIAN ON THE BACK OF EXISTING R4-7 SIGN	MO 4-8	24"x12"	1	49	49						
	"	M 3-4	24"x12"	1	49	49						
	"	M 1-6	24"x24"	1	49	49						96
	"	MO 6-1	21"x21"	1	49	49						LEFT
37	CTH U, S. OF I-41, PLACE RIGHT OF EXISTING M1-5A S. OF FRONTAGE RD INTERSECTION	MO 4-8	24"x12"	1	49	49						
	"	M 3-2	24"x12"	1	49	49						
	"	M 1-6	24"x24"	1	49	49						96
38	CTH U, N. OF GOLF COURSE DR, PLACE 150' N. OF GOLF COURSE DR RAB INTERSECTION	MO 4-8	24"x12"	1	49	49						
	"	M 3-2	24"x12"	1	49	49						
	"	M 1-6	24"x24"	1	49	49						96
	"	MO 6-1	21"x21"	1	49	49						AHEAD
39	CTH U, S. OF GOLF COURSE DR, PLACE 150' S. OF GOLF COURSE DR RAB INTERSECTION	MO 4-8	24"x12"	1	49	49						
	"	M 3-4	24"x12"	1	49	49						
	"	M 1-6	24"x24"	1	49	49						96
	"	MO 6-1	21"x21"	1	49	49						AHEAD
PAGE SUBTOTALS				31		1,519	0	0	0		0	

*ADDITIONAL QUANTITIES SHOWN ELSEWHERE

SHEET INFORMATION PROVIDED BY WISDOT

TRAFFIC CONTROL DETOUR SIGN SUMMARY - PROJECT 4075-35-71 CONTINUED

SIGN NO.	LOCATION	SIGN CODE	SIZE W X H	NUMBER IN SERVICE	APPROX. SERVICE PERIOD 49 DAYS	* 643.0900 SIGNS DAYS	* 643.0420 BARRICADES TYPE III DAYS	* 643.0705 WARNING LIGHTS TYPE A DAYS	* 643.1050 SIGNS PORTABLE CHANGEABLE MESSAGE DAYS	NO OF CYCLES	* 643.0920 COVERING SIGNS TYPE II EACH	REMARKS
40	CTH U, N. OF STH 96, MODIFY EXISTING J1-1 SIGN AS SHOWN	M 4-8A	24"x18"	1	49	49						
41	CTH U, AT STH 96, MODIFY EXISTING J13-1 SIGN AS SHOWN	MO 6-1	21"x21"	1	49	49						LEFT
42	STH 96, AT CTH U, PLACE IN ROADWAY AT CTH U INTERSECTION	R 11-4	60"x30"	1	49	49	49	98				
	"	M 4-9R	30"x24"	1	49	49						
43	STH 96, W. OF CTH U, PLACE ON RIGHT SHOULDER, FIELD DETERMINE LOCATION	PCMS		1					7			PLACE IN ADVANCE OF CLOSURE
44	STH 96, AT CTH U, PLACE RIGHT OF EXISTING J3-2 SIGN AT CTH U INTERSECTION	MO 4-8	24"x12"	1	49	49						
	"	M 3-4	24"x12"	1	49	49						
	"	M 1-6	24"x24"	1	49	49						96
	"	MO 6-1	21"x21"	1	49	49						RIGHT
45	STH 96, E. OF CTH U, PLACE 500' E. OF CTH U INTERSECTION	MO 4-8	24"x12"	1	49	49						
	"	M 3-4	24"x12"	1	49	49						
	"	M 1-6	24"x24"	1	49	49						96
	"	MO 5-1R	21"x21"	1	49	49						
46	STH 96, E. OF CTH U, PLACE 1500' E. OF CTH U INTERSECTION	W 20-2A	48"x48"	1	49	49						
PAGE SUBTOTALS				14		637	49	98	7		0	
DETOUR 4075-35-71 TOTALS				123		5,929	147	294	14		3	

*ADDITIONAL QUANTITIES SHOWN ELSEWHERE

SHEET INFORMATION PROVIDED BY WISDOT

TRAFFIC CONTROL DETOUR SIGN SUMMARY - PROJECT 4075-35-72

3

3

SIGN NO.	LOCATION	SIGN CODE	SIZE W X H	NUMBER IN SERVICE	APPROX. SERVICE PERIOD DAYS	* 643.0900 SIGNS DAYS	* 643.0420 BARRICADES TYPE III DAYS	* 643.0705 WARNING LIGHTS TYPE A DAYS	643.1000 SIGNS FIXED MESSAGE SF	* 643.1050 SIGNS PORTABLE CHANGEABLE MESSAGE DAYS	NO OF CYCLES	* 643.0920 COVERING SIGNS TYPE II EACH	REMARKS
1	STH 96, W. OF CTH U, PLACE 1500' W. OF CTH U INTERSECTION	W 20-2A	48"x48"	1	31	31							
2	STH 96, W. OF CTH U, PLACE 1000' W. OF CTH U INTERSECTION	FMS	78"x48"	1					26				SEE SIGN DETAIL SHEET
3	STH 96, W. OF CTH U, PLACE 500' W. OF CTH U INTERSECTION	MO 4-8	24"x12"	1	31	31							
	"	M 3-2	24"x12"	1	31	31							
	"	M 1-6	24"x24"	1	31	31							96
	"	MO 5-1L	21"x21"	1	31	31							
4	STH 96, AT CTH U, PLACE RIGHT OF EXISTING J13-1 SIGN	MO 4-8	24"x12"	1	31	31							
	"	M 3-2	24"x12"	1	31	31							
	"	M 1-6	24"x24"	1	31	31							96
	"	MO 6-1	21"x21"	1	31	31							LEFT
5	STH 96, AT CTH U, PLACE ON SHOULDER IN SE QUADRANT OF INTERSECTION	R 11-4	60"x30"	1	31	31	31	62					
	"	M 4-9L	30"x24"	1	31	31							
6	STH 96, E. OF CTH U, COVER EXISTING J4-1 SIGN AS SHOWN										1	1	COVER ENTIRE SIGN
7	STH 96, E. OF CTH U, PLACE ON SHOULDER, FIELD DETERMINE LOCATION	PCMS		1						7			PLACE IN ADVANCE OF CLOSURE
8	CTH U, AT STH 96, MODIFY EXISTING J13-1 SIGN AS SHOWN	MO 6-1	21"x21"	1	31	31							RIGHT
9	CTH U, N. OF STH 96, PLACE 250' N. OF STH 96 INTERSECTION	MO 4-8	24"x12"	1	31	31							
	"	M 3-2	24"x12"	1	31	31							
	"	M 1-6	24"x24"	1	31	31							96
10	CTH U, N. OF STH 96, MODIFY EXISTING J1-1 SIGN AS SHOWN	M 4-8A	24"x18"	1	31	31							
11	CTH U, S. OF CTH DD, PLACE 500' S. OF CTH DD RAB INTERSECTION	MO 4-8	24"x12"	1	31	31							
	"	M 3-2	24"x12"	1	31	31							
	"	M 1-6	24"x24"	1	31	31							96
	"	MO 5-1R	21"x21"	1	31	31							
12	CTH U, S. OF CTH DD, PLACE 250' S. OF CTH DD RAB INTERSECTION	MO 4-8	24"x12"	1	31	31							
	"	M 3-4	24"x12"	1	31	31							
	"	M 1-6	24"x24"	1	31	31							96
13	CTH U, AT CTH DD, PLACE RIGHT OF EXISTING R1-2 SIGN AT RAB INTERSECTION	MO 4-8	24"x12"	1	31	31							
	"	M 3-2	24"x12"	1	31	31							
	"	M 1-6	24"x24"	1	31	31							96
	"	MO 6-1	21"x21"	1	31	31							RIGHT
14	CTH U, AT CTH DD, PLACE ABOVE EXISTING SPLITTER ISLAND SIGN ON SOUTH LEG	MO 4-8	24"x12"	1	31	31							
	"	M 3-4	24"x12"	1	31	31							
	"	M 1-6	24"x24"	1	31	31							96
	"	MO 6-2	21"x21"	1	31	31							TILT RIGHT
15	CTH DD, AT CTH U, PLACE ABOVE EXISTING SPLITTER ISLAND SIGN ON EAST LEG	MO 4-8	24"x12"	1	31	31							
	"	M 3-2	24"x12"	1	31	31							
	"	M 1-6	24"x24"	1	31	31							96
	"	MO 6-2	21"x21"	1	31	31							TILT RIGHT
16	CTH DD, AT CTH U, PLACE RIGHT OF EXISTING R1-2 SIGN AT RAB INTERSECTION	MO 4-8	24"x12"	1	31	31							
	"	M 3-4	24"x12"	1	31	31							
	"	M 1-6	24"x24"	1	31	31							96
	"	MO 6-1	21"x21"	1	31	31							LEFT

PAGE SUBTOTALS

41

1,209

31

62

26

7

1

*ADDITIONAL QUANTITIES SHOWN ELSEWHERE

SHEET INFORMATION PROVIDED BY WISDOT

TRAFFIC CONTROL DETOUR SIGN SUMMARY - PROJECT 4075-35-72 - CONTINUED

SIGN NO.	LOCATION	SIGN CODE	SIZE W X H	NUMBER IN SERVICE	APPROX. SERVICE PERIOD 31 DAYS	* 643.0900	* 643.0420	* 643.0705	643.1000	* 643.1050	NO OF CYCLES	* 643.0920	REMARKS
						SIGNS	BARRICADES TYPE III	WARNING LIGHTS TYPE A	SIGNS FIXED MESSAGE SF	SIGNS PORTABLE CHANGEABLE MESSAGE DAYS		COVERING SIGNS TYPE II EACH	
17	CTH DD, E. OF CTH U, PLACE 250' E. OF CTH U RAB INTERSECTION	MO 4-8	24"x12"	1	31	31							
	"	M 3-2	24"x12"	1	31	31							
	"	M 1-6	24"x24"	1	31	31							96
18	CTH DD, E. OF CTH U, PLACE 500' E. OF CTH U RAB INTERSECTION	MO 4-8	24"x12"	1	31	31							
	"	M 3-4	24"x12"	1	31	31							
	"	M 1-6	24"x24"	1	31	31							96
	"	MO 5-1L	21"x21"	1	31	31							
19	CTH DD, W. OF STH 96, MODIFY EXISTING J1-1 SIGN AS SHOWN	M 4-8A	24"x18"	1	31	31							
20	CTH DD, W. OF STH 96, PLACE 250' W. OF STH 96 RAB INTERSECTION	MO 4-8	24"x12"	1	31	31							
	"	M 3-4	24"x12"	1	31	31							
	"	M 1-6	24"x24"	1	31	31							96
21	CTH DD, AT STH 96, PLACE RIGHT OF EXISTING R1-2 SIGN AT RAB INTERSECTION	M 1-6	24"x24"	1	31	31							96
	"	MO 6-1	21"x21"	1	31	31							AHEAD
22	CTH DD, AT STH 96, PLACE ABOVE EXISTING SPLITTER ISLAND SIGN ON NORTH LEG	MO 4-8	24"x12"	1	31	31							
	"	M 3-4	24"x12"	1	31	31							
	"	M 1-6	24"x24"	1	31	31							96
	"	MO 6-2	21"x21"	1	31	31							TILT RIGHT
23	STH 96, AT CTH D/DD, COVER EXISTING SPLITTER ISLAND SIGN ON WEST LEG AS SHOWN										1	1	COVER ENTIRE SIGN
24	STH 96, AT CTH D/DD, PLACE ON RIGHT SHOULDER IN NW QUADRANT OF RAB INTERSECTION	R 11-4	60"x30"	1	31	31	31	62					
	"	M 4-9R	30"x24"	1	31	31							
25	STH 96, W. OF CTH D/DD, PLACE ON SHOULDER, FIELD DETERMINE LOCATION	PCMS		1						7			PLACE IN ADVANCE OF CLOSURE
26	STH 96, AT CTH D/DD, PLACE RIGHT OF EXISTING R1-2 SIGN AT RAB INTERSECTION	MO 4-8	24"x12"	1	31	31							
	"	M 3-4	24"x12"	1	31	31							
	"	M 1-6	24"x24"	1	31	31							96
	"	MO 6-1	21"x21"	1	31	31							AHEAD
27	STH 96, E. OF CTH D/DD, MODIFY EXISTING J3-1 SIGN AS SHOWN	MO 6-1	21"x21"	1	31	31							AHEAD
	"	MO 6-1	21"x21"	1	31	31							AHEAD
28	STH 96, E. OF CTH D/DD, PLACE 1000' E. OF CTH D/DD INTERSECTION	W 20-2A	48"x48"	1	31	31							
PAGE SUBTOTALS				27		806	31	62	0	7		1	
DETOUR 4075-35-72 TOTALS				68		2,015	62	124	26	14		2	

*ADDITIONAL QUANTITIES SHOWN ELSEWHERE

SHEET INFORMATION PROVIDED BY WISDOT

MARKING

CATEGORY	STATION TO	STATION	LOCATION	643.3165		646.2020		646.2040		646.4040		646.7420		REMARKS
				TEMPORARY MARKING LINE PAINT 6-INCH YELLOW LF	TEMPORARY MARKING LINE WHITE LF	MARKING LINE EPOXY 6-INCH YELLOW LF	MARKING LINE WHITE LF	MARKING LINE GROOVED WET REFLECTIVE EPOXY 6-INCH YELLOW LF	MARKING LINE GROOVED WET REF EPOXY 10-INCH WHITE LF	MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH WHITE LF				
PROJECT 4075-35-71														
0010	33+50	-	40+00	STH 96	-	-	1,300	-	-	1,250	-	-	-	
0010	40+00	-	64+30	STH 96	6,140	-	3,400	-	-	4,660	-	-	-	TEMPORARY MARKED 2 TIMES
0010	64+30	-	104+00	STH 96	8,580	-	-	-	4,970	7,830	-	-	-	TEMPORARY MARKED 2 TIMES
0010	104+00	-	129+04	STH 96	5,410	-	-	-	3,130	4,760	-	-	-	TEMPORARY MARKED 2 TIMES
0010			LAMPLIGHTER DR		-	-	-	-	-	-	220	-	78	INTERSECTION
0010			FARMLAND DR		-	-	-	-	-	-	220	-	-	INTERSECTION
0010			FOX MEADOWS LN		-	-	-	-	-	-	120	-	-	INTERSECTION
0010			KELSO RD		-	-	-	-	-	-	120	-	-	INTERSECTION
0010			RIVER FOREST DR		-	-	-	-	-	-	100	-	-	INTERSECTION
0010			STH 96		170	-	-	-	-	-	-	-	-	CULVERT REPLACEMENTS
PROJECT SUBTOTALS					20,300	0	4,700	0	8,100	18,500	780	-	78	
PROJECT TOTALS					20,300		4,700		26,600		780		78	
PROJECT 4075-35-72														
0010	129+04	-	145+00	STH 96	9,600	-	-	-	3,200	2,950	-	-	-	TEMPORARY MARKED 3 TIMES
0010	145+00	-	253+25	STH 96	10,740	-	-	-	5,400	21,150	-	-	-	TEMPORARY MARKED 3 TIMES
0010	253+25	-	291+71	STH 96	-	-	7,600	-	-	-	-	-	-	
0010			CTH JJ		-	-	100	-	-	-	250	-	-	INTERSECTION
0010			CTH U		-	-	300	-	-	-	200	-	-	INTERSECTION
0010			VAN DYKE ST		-	-	-	-	-	-	-	-	84	INTERSECTION
0010			NANCY ST		-	-	-	-	-	-	-	-	251	INTERSECTION
0010			LINDA ST		-	-	-	-	-	-	-	-	346	INTERSECTION
0010			2ND STREET		-	-	-	-	-	-	-	-	117	INTERSECTION
0010			1ST STREET		-	-	-	-	-	-	-	-	148	INTERSECTION
0010			OAK ST		-	-	-	-	-	-	-	-	183	INTERSECTION
0010			POPLAR ST		-	-	-	-	-	-	-	-	189	INTERSECTION
0010			GREEN ST		-	-	-	-	-	-	-	-	148	INTERSECTION
0010			STH 96		260	-	-	-	-	-	-	-	-	CULVERT REPLACEMENTS
PROJECT SUBTOTALS					20,600	0	8,000	0	8,600	24,100	450	-	1,466	
PROJECT TOTALS					20,600		8,000		32,700		450		1,466	
TOTALS 0010					40,900		12,700		59,300		1,230		1,544	

MATCH EXISTING CENTERLINE PASSING ZONES. 4-FT SKIPS FOR PASSING ZONE TEMPORARY MARKING LINE.

STAKING

CATEGORY	STATION TO	STATION	LOCATION	650.4000	650.5500	650.6000	650.8000	650.9000	650.9500.01	650.9500.02	650.9911.01	650.9911.02	
				CONSTRUCTION STAKING STORM SEWER EACH	CONSTRUCTION STAKING CURB GUTTER AND CURB & GUTTER LF	CONSTRUCTION STAKING PIPE CULVERTS EACH	CONSTRUCTION STAKING RESURFACING REFERENCE LF	CONSTRUCTION STAKING CURB RAMPS EACH	CONSTRUCTION STAKING SIDEWALK 4075-35-71 EACH	CONSTRUCTION STAKING SIDEWALK 4075-35-72 EACH	CONSTRUCTION STAKING SUPPLEMENTAL CONTROL 4075-35-71 EACH	CONSTRUCTION STAKING SUPPLEMENTAL CONTROL 4075-35-72 EACH	
PROJECT 4075-35-71													
0010	33+50	-	129+04	STH 96	-	57	2	9,554	2	1	-	1	-
PROJECT TOTALS					0	57	2	9,554	2	1	0	1	0
PROJECT 4075-35-71													
0010	129+04	-	291+71	STH 96	4	1,024	4	16,267	25	-	1	-	1
PROJECT TOTALS					4	1,024	4	16,267	25	0	1	0	1
TOTALS 0010					4	1,081	6	25,821	27	1	1	1	1

TEMPORARY PEDESTRIAN TRAFFIC CONTROL

CATEGORY	LOCATION	644.1440 TEMPORARY PEDESTRIAN SURFACE MATTING SF	644.1601 TEMPORARY PEDESTRIAN CURB RAMP DAY	644.1605 TEMPORARY PEDESTRIAN DETECTABLE WARNING FIELD SF	REMARKS
PROJECT 4075-35-72					
STAGE 3A					
0010	STH 96, LT	40	10	10	POPULAR ST NW QUAD
PROJECT TOTAL		40	10	10	
TOTAL 0010		40	10	10	

TEMPORARY PEDESTRIAN BARRICADE

CATEGORY	LOCATION	644.1810 TEMPORARY PEDESTRIAN BARRICADE LF	REMARKS
PROJECT 4075-35-72			
STAGE A			
0010	STH 96, LT	18	NANCY ST NW QUAD
0010	STH 96, LT	18	NANCY ST NE QUAD
0010	STH 96, LT	36	2ND ST NE QUAD
0010	STH 96, LT	36	OAK ST NW QUAD
0010	STH 96, LT	36	GREEN ST NW QUAD
STAGE B			
0010	STH 96, LT	18	VAN DYKE RD NE QUAD
0010	STH 96, LT	18	VAN DYKE RD SE QUAD
0010	STH 96, LT	18	LINDA ST NW QUAD
0010	STH 96, LT	18	LINDA ST NE QUAD
0010	STH 96, LT	36	1ST ST NE QUAD
0010	STH 96, LT	102	POPLAR ST NW QUAD
STAGE C			
0010	STH 96, RT	18	NANCY ST SW QUAD
0010	STH 96, RT	18	NANCY ST SE QUAD
0010	STH 96, RT	18	2ND ST SW QUAD
0010	STH 96, RT	18	2ND ST SE QUAD
0010	STH 96, RT	36	OAK ST SW QUAD
0010	STH 96, LT	18	POPLAR ST NE QUAD
0010	STH 96, LT	18	GREEN ST NE QUAD
STAGE D			
0010	STH 96, RT	18	LINDA ST SW QUAD
0010	STH 96, RT	18	LINDA ST SE QUAD
0010	STH 96, RT	18	1ST ST SW QUAD
0010	STH 96, RT	18	1ST ST SE QUAD
0010	STH 96, RT	36	POPLAR ST SW QUAD
STAGE E			
temp			
0010	STH 96, RT	18	OAK ST SE QUAD
0010	STH 96, RT	36	GREEN ST SW QUAD
PROJECT TOTAL		660	
TOTAL 0010		660	

SAWING

CATEGORY	STATION	TO	STATION	LOCATION	690.0150 SAWING ASPHALT LF	690.0250 SAWING CONCRETE LF	REMARKS
PROJECT 4075-35-71							
0010	44+79	-	44+87	STH 96, LT	31	11	LAMPLIGHTER DR SW QUAD
0010	45+17	-	45+40	STH 96, LT	40	11	LAMPLIGHTER DR NW QUAD
0010	51+90	-	52+10	STH 96, LT	50	-	CULVERT PIPE
0010	118+35	-	119+42	STH 96	60	-	CULVERT PIPE
PROJECT TOTALS					181	22	
PROJECT 4075-35-72							
0010	163+49	-	164+66	STH 96	64	-	CULVERT PIPE
0010	177+76	-	178+95	STH 96	64	-	CULVERT PIPE
0010	216+56	-	218+52	STH 96	64	-	CULVERT PIPE
0010	251+02	-	251+22	STH 96, RT	52	-	CULVERT PIPE
0010	254+24	-	254+62	STH 96, LT	50	10	VAN DYKE ST NE QUAD
0010	254+16	-	254+46	STH 96, RT	41	10	VAN DYKE ST SE QUAD
0010	260+70	-	262+49	STH 96	382	-	STORM SEWER REPLACEMENT
0010	262+39	-	262+74	STH 96, LT	53	10	NANCY ST NW QUAD
0010	263+05	-	263+34	STH 96, LT	39	10	NANCY ST NE QUAD
0010	262+35	-	262+61	STH 96, RT	30	15	NANCY ST SW QUAD
0010	263+07	-	263+32	STH 96, RT	29	15	NANCY ST SE QUAD
0010	271+11	-	271+42	STH 96, LT	52	10	LINDA ST NW QUAD
0010	271+70	-	271+99	STH 96, LT	44	10	LINDA ST NE QUAD
0010	271+09	-	271+40	STH 96, RT	51	10	LINDA ST SW QUAD
0010	271+69	-	272+00	STH 96, RT	49	10	LINDA ST SE QUAD
0010	271+40	-	273+80	STH 96, LT	307	3	STORM SEWER REPLACEMENT
0010	274+55	-	274+70	STH 96, RT	37	10	2ND ST SW QUAD
0010	274+78	-	275+06	STH 96, RT	44	10	2ND ST SE QUAD
0010	274+80	-	275+06	STH 96, LT	30	15	2ND ST NE QUAD
0010	277+42	-	277+63	STH 96, RT	50	10	1ST ST SW QUAD
0010	277+96	-	278+36	STH 96, RT	55	10	1ST ST SE QUAD
0010	278+98	-	278+33	STH 96, LT	40	15	1ST ST NE QUAD
0010	283+73	-	283+85	STH 96, LT	24	-	STORM SEWER REPLACEMENT
0010	283+77	-	283+96	STH 96, RT	42	15	OAK ST SW QUAD
0010	284+12	-	284+34	STH 96, RT	37	10	OAK ST SE QUAD
0010	283+70	-	284+03	STH 96, LT	38	15	OAK ST NW QUAD
0010	285+09	-	285+35	STH 96, LT	37	15	POPLAR ST NW QUAD
0010	285+75	-	286+05	STH 96, LT	42	10	POPLAR ST NE QUAD
0010	285+15	-	285+34	STH 96, RT	23	15	POPLAR ST SE QUAD
0010	289+79	-	290+08	STH 96, RT	47	15	GREEN ST SW QUAD
0010	290+31	-	390+53	STH 96, RT	39	10	GREEN ST SE QUAD
0010	289+87	-	290+16	STH 96, LT	33	15	GREEN ST NW QUAD
PROJECT TOTALS					1,989	303	
TOTAL 0010					2,170	325	

TRANSPORTATION PROJECT PLAT NO: 4075-35-21-4.01

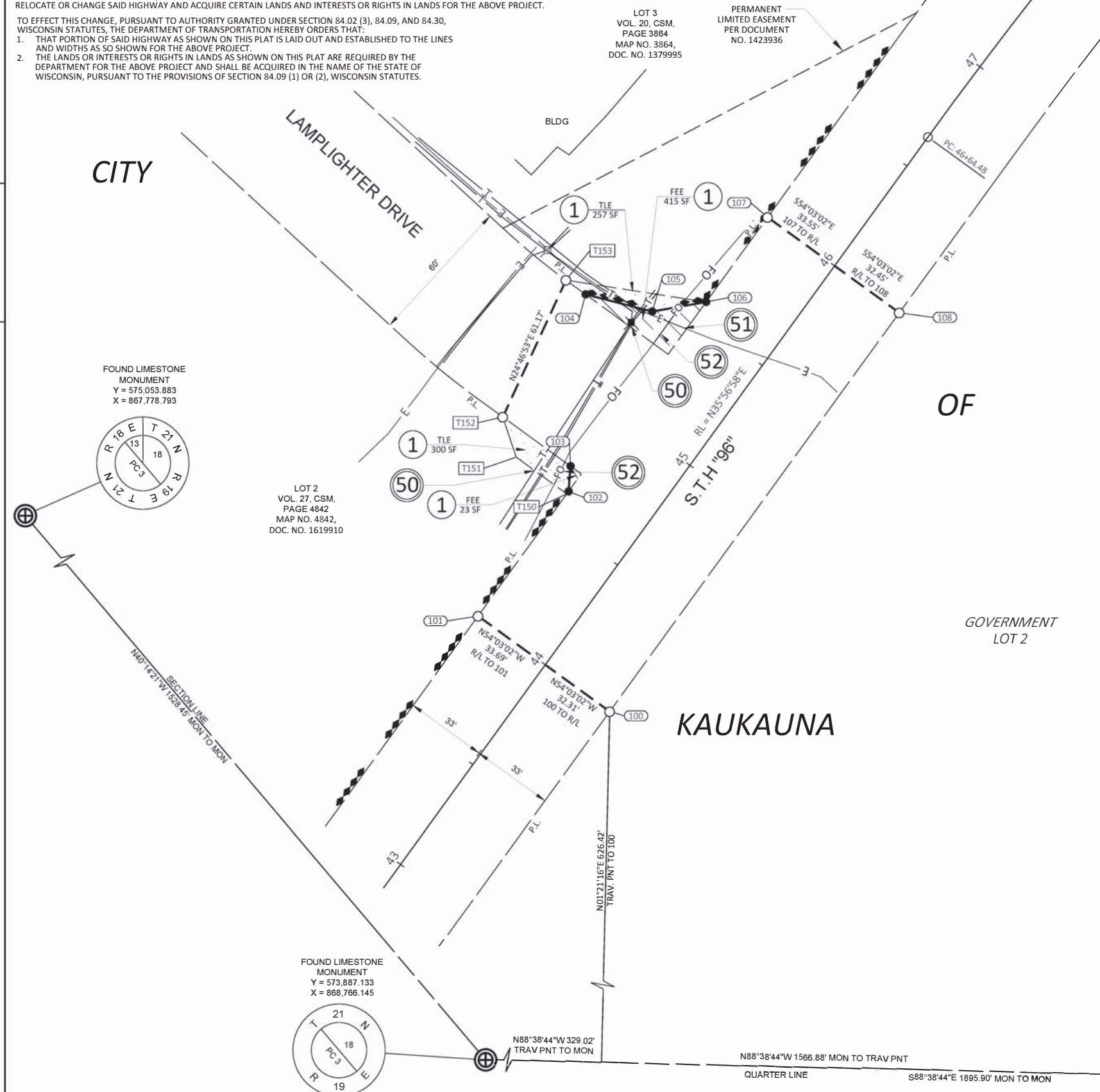
THAT PART OF LOT 2, VOLUME 27 OF CERTIFIED SURVEY MAPS, PAGE 4842, MAP NO. 4842, DOC. NO. 1619910, AND THAT PART OF LOT 3, VOLUME 20 OF CERTIFIED SURVEY MAPS, PAGE 3864, MAP NO. 3864, DOC. NO. 1379995, ALL LOCATED IN GOVERNMENT LOT 2 OF SECTION 18, TOWNSHIP 21 NORTH, RANGE 19 EAST, IN THE CITY OF KAUKAUNA, OUTAGAMIE COUNTY, WISCONSIN.

RELOCATION ORDER STH 96, KAUKAUNA - WRIGHTSTOWN, CLARIBEL STREET - CTH D - OUTAGAMIE COUNTY - BROWN COUNTY

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE PROJECT.

TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SECTION 84.02 (3), 84.09, AND 84.30, WISCONSIN STATUTES, THE DEPARTMENT OF TRANSPORTATION HEREBY ORDERS THAT:

- THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS LAID OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE PROJECT.
- THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE DEPARTMENT FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE STATE OF WISCONSIN, PURSUANT TO THE PROVISIONS OF SECTION 84.09 (1) OR (2), WISCONSIN STATUTES.



SCHEDULE OF LANDS & INTERESTS REQUIRED						
PARCEL NUMBER	OWNER(S)	INTEREST(S) REQUIRED	R/W SQUARE FEET			TLE SF
			NEW	EXISTING	TOTAL	
1	LAMPLIGHT, LLC	FEE, TLE	438	-----	438	557

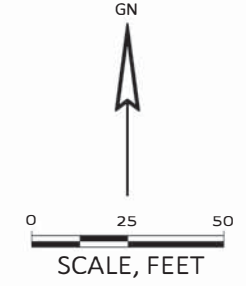
OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE DEPARTMENT.
PURPOSE OF ALL THE TLE'S ARE FOR GRADING PURPOSES, UNLESS NOTED.

COURSE TABLE		
COURSE	BEARING	DISTANCE
101-102	N35°59'18\"E	62.50'
102-103	N04°31'37\"E	10.27'
103-T152	N54°00'56\"W	34.00'
T153-104	S54°00'56\"E	9.85'
104-105	S75°35'28\"E	27.95'
105-106	N79°59'08\"E	22.18'
106-107	N35°59'18\"E	42.50'
108-100	S35°59'18\"W	200.00'

STATION & OFFSET TABLE		
POINT	STATION	OFFSET
T150	44+61.26	33.64'LT
T151	44+61.29	59.00'LT
T152	44+71.29	73.00'LT
T153	45+31.29	84.85'LT

PI STA = 47+46.98
Y = 574805.465
X = 869287.419
DELTA = 4°56'49\" RT
D = 3°00'00\"
T = 82.50'
L = 164.90'
R = 1909.86'
PC STA = 46+64.48
Y = 574738.679
X = 869238.986
PT STA = 48+29.38
Y = 574867.826
X = 869341.430
DB = N35°56'58\"E
DA = N40°53'47\"E

STATION & OFFSET TABLE		
POINT	STATION	OFFSET
100	44+00.00	32.31'RT
101	44+00.00	33.69'LT
102	44+62.50	33.64'LT
103	44+71.27	39.00'LT
104	45+31.29	75.00'LT
105	45+41.55	49.00'LT
106	45+57.50	33.58'LT
107	46+00.00	33.55'LT
108	46+00.00	32.45'RT



- 50 ~~AT&T~~ NO EASEMENT OF RECORD - PARCELS 1&2
 - 51 ~~WE ENERGIES - ELECTRIC~~ NO EASEMENT OF RECORD - PARCEL 2
 - 52 ~~CENTURYLINK~~ NO EASEMENT OF RECORD - PARCELS 1&2
- See Affidavit of Correction recorded as Doc# 2265341.

UTILITY INTERESTS REQUIRED		
UTILITY NO.	OWNER(S)	INTEREST REQUIRED
50	AT&T	RELEASE OF RIGHTS
51	WE ENERGIES - ELECTRIC	RELEASE OF RIGHTS
52	CENTURYLINK	RELEASE OF RIGHTS

NOTES:
POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), OUTAGAMIE COUNTY, NAD83(2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

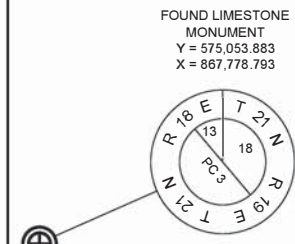
ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 1 X 24\" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

FOR THE LATEST ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN GREEN BAY.

EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE:
STH 96: RIGHT OF WAY PROJECT NUMBER 4076-3-21, CSM #4842, CSM #3864.
LAMPLIGHTER DRIVE: CSM #4842, CSM #3864.

ACCESS RESTRICTIONS PER RIGHT OF WAY CSM #4842, AND CSM #3864.

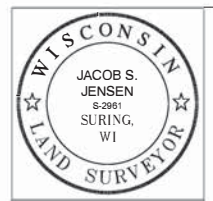
FOR ADDITIONAL INFORMATION REFER TO THE TITLE SHEET, RECORDED AS SHEET 2 OF 2.



LOT 2
VOL. 27, CSM,
PAGE 4842
MAP NO. 4842,
DOC. NO. 1619910

FOUND LIMESTONE
MONUMENT
Y = 573,887.133
X = 868,766.145

COMPUTED LOCATION
USING TIES
Y = 573,842.321
X = 870,661.512



I, JACOB S. JENSEN, PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION, I HAVE SURVEYED AND MAPPED THIS TRANSPORTATION PROJECT PLAT 4075-35-21-4.01 AND THAT SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND AND.

JACOB S. JENSEN
REGISTRATION NUMBER: S-2961
NOVEMBER 19, 2021
DATE

THIS PLAT AND RELOCATION ORDER ARE APPROVED FOR THE WISCONSIN DEPARTMENT OF TRANSPORTATION, NORTHEAST REGION.

CURT VAN EREM
NOVEMBER 19, 2021
DATE

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION TRANSPORTATION PROJECT PLAT TITLE SHEET

4075-35-21

KAUKAUNA - WRIGHTSTOWN

CLARIBEL STREET - CTH D

STH 96

OUTAGAMIE COUNTY - BROWN COUNTY



PROJECT LOCATION

CONVENTIONAL SYMBOLS

SECTION LINE	---	SECTION CORNER SYMBOL		R/W MONUMENT (TO BE SET)	●
QUARTER LINE	---	SECTION CORNER MONUMENT		NON-MONUMENTED R/W POINT	○
SIXTEENTH LINE	---	GEODETIC SURVEY MONUMENT		FOUND IRON PIN (1-INCH UNLESS NOTED)	IP
NEW REFERENCE LINE	---	SIXTEENTH CORNER MONUMENT		OFF-PREMISE SIGN	
NEW R/W LINE	---	SIGN		COMPENSABLE	
EXISTING R/W OR HE LINE	---	OFF-PREMISE SIGN		NON-COMPENSABLE	
PROPERTY LINE	---	PARCEL NUMBER		UTILITY NUMBER	
LOT, TIE & OTHER MINOR LINES	---	PARALLEL OFFSETS			
SLOPE INTERCEPT	---				
CORPORATE LIMITS	---				
UNDERGROUND FACILITY (COMMUNICATIONS, ELECTRIC, ETC.)	---				
NEW R/W (FEE OR HE) (HATCHING VARIES BY OWNER)	---				
TEMPORARY LIMITED EASEMENT AREA	---				
EASEMENT AREA (PERMANENT LIMITED OR RESTRICTED DEVELOPMENT)	---				
TRANSMISSION STRUCTURES	---				
BUILDING		TO BE REMOVED			
BRIDGE					

CONVENTIONAL ABBREVIATIONS

ACCESS RIGHTS	AR	POINT OF INTERSECTION	PI
ACRES	AC	PROPERTY LINE	PL
AHEAD	AH	RECORDED AS	(100')
ALUMINUM	ALUM	REEL / IMAGE	R/I
AND OTHERS	ET AL	REFERENCE LINE	R/L
BACK	BK	REMAINING	REM
BLOCK	BLK	RESTRICTIVE DEVELOPMENT EASEMENT	RDE
CENTERLINE	C/L		
CERTIFIED SURVEY MAP	CSM	RIGHT	RT
CONCRETE	CONC	RIGHT OF WAY	R/W
COUNTY	CO	SECTION	SEC
COUNTY TRUNK HIGHWAY	CTH	SEPTIC VENT	SEPV
DISTANCE	DIST	SQUARE FEET	SF
CORNER	COR	STATE TRUNK HIGHWAY	STH
DOCUMENT NUMBER	DOC	STATION	STA
EASEMENT	EASE	TELEPHONE PEDESTAL	TP
EXISTING	EX	TEMPORARY LIMITED EASEMENT	TLE
GAS VALVE	GV		
GRID NORTH	GN	TRANSPORTATION PROJECT	TPP
HIGHWAY EASEMENT	HE	PLAT	
IDENTIFICATION	ID	UNITED STATES HIGHWAY	USH
LAND CONTRACT	LC	VOLUME	V
LEFT	LT		
MONUMENT	MON		
NATIONAL GEODETIC SURVEY	NGS		
NUMBER	NO		
OUTLOT	OL		
PAGE	P		
POINT OF TANGENCY	PT		
PERMANENT LIMITED EASEMENT	PLE		
POINT OF BEGINNING	POB		
POINT OF CURVATURE	PC		
POINT OF COMPOUND CURVE	PCC		

CURVE DATA

LONG CHORD	LCH
LONG CHORD BEARING	LCB
RADIUS	R
DEGREE OF CURVE	D
CENTRAL ANGLE	Δ/DELTA
LENGTH OF CURVE	L
TANGENT	T
DIRECTION AHEAD	DA
DIRECTION BACK	DB

CONVENTIONAL UTILITY SYMBOLS

---	WATER
---	GAS
---	TELEPHONE
---	OVERHEAD TRANSMISSION LINES
---	ELECTRIC
---	CABLE TELEVISION
---	FIBER OPTIC
---	SANITARY SEWER
---	STORM SEWER



PROJECT LOCATION

THE NOTES, CONVENTIONAL SIGNS, AND ABBREVIATIONS ARE ASSOCIATED WITH EACH TRANSPORTATION PROJECT PLAT FOR PROJECT 4075-35-21.

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), OUTAGAMIE COUNTY, NAD83(2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 1" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

ALL RIGHT-OF-WAY LINES DEPICTED IN THE NON-ACQUISITION AREAS ARE INTENDED TO RE-ESTABLISH EXISTING RIGHT-OF-WAY LINES AS DETERMINED FROM PREVIOUS PROJECTS, OTHER RECORDED DOCUMENTS, OR FROM CENTERLINE OF EXISTING PAVEMENTS.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS" OF PUBLIC RECORD.

DIMENSIONING FOR THE NEW RIGHT-OF-WAY IS MEASURED ALONG AND PERPENDICULAR TO THE NEW REFERENCE LINES.

A TEMPORARY LIMITED EASEMENT (TLE) IS A RIGHT FOR CONSTRUCTION PURPOSES, AS DEFINED HEREIN, INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON, THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM DESIRABLE. ALL (TLEs) ON THIS PLAT EXPIRE AT THE COMPLETION OF THE CONSTRUCTION PROJECT FOR WHICH THIS INSTRUMENT IS GIVEN.

PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM DATA DERIVED FROM MAPS AND DOCUMENTS OF PUBLIC RECORD AND/OR EXISTING OCCUPANCIAL LINES. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.

FOR THE LATEST ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN GREEN BAY.

PARCEL AND IDENTIFICATION NUMBERS MAY NOT POINT TO ALL AREAS OF ACQUISITION, AS NOTED ON THE TPP DETAIL PAGES.

INFORMATION FOR THE BASIS OF EXISTING HIGHWAY RIGHT-OF-WAY POINTS OF REFERENCE AND ACCESS CONTROL ARE LISTED ON THE TPP DETAIL PAGES.

PROJECT NUMBER 4075-35-21 -4.01
SHEET 2 OF 2
AMENDMENT NO:

TRANSPORTATION PROJECT PLAT NO: 4075-35-21-4.02

THAT PART OF LOT 34, FIRST ADDITION TO RIVER FOREST ESTATES, DOCUMENT NUMBER 945090, LOCATED IN THE SOUTHWEST 1/4 OF THE SOUTHEAST 1/4 OF SECTION 7, TOWNSHIP 21 NORTH, RANGE 19 EAST, IN THE TOWN OF KAUKAUNA, OUTAGAMIE COUNTY, WISCONSIN.

RELOCATION ORDER STH 96, KAUKAUNA - WRIGHTSTOWN, CLARIBEL STREET - CTH D - OUTAGAMIE COUNTY - BROWN COUNTY

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE PROJECT.

TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SECTION 84.02 (3), 84.09, AND 84.30, WISCONSIN STATUTES, THE DEPARTMENT OF TRANSPORTATION HEREBY ORDERS THAT:

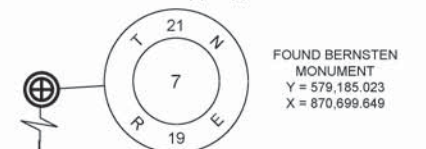
- THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS LAID OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE PROJECT.
- THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE DEPARTMENT FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE STATE OF WISCONSIN, PURSUANT TO THE PROVISIONS OF SECTION 84.09 (1) OR (2), WISCONSIN STATUTES.

SCHEDULE OF LANDS & INTERESTS REQUIRED						
PARCEL NUMBER	OWNER(S)	INTEREST(S) REQUIRED	R/W SQUARE FEET			
			NEW	EXISTING	TOTAL	TLE
3	DNA VENTURES LLC	FEE	1102	-----	1102	-----

OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE DEPARTMENT.
PURPOSE OF ALL THE TLE'S ARE FOR GRADING PURPOSES, UNLESS NOTED.

Document #: 2257067
Date: 12-16-2021 Time: 2:33 PM
Pages: 1 Fee: \$25.00
County: OUTAGAMIE COUNTY State: WI
Jacob A. Jensen
SARAH R VAN CAMP, REGISTER OF DEEDS
This document has been electronically recorded
Return to:
WisDOT - NE Region - Green Bay - PO# 39500-0000

RESERVED FOR REGISTER OF DEEDS
PROJECT NUMBER 4075-35-21-4.02
SHEET 1 OF 1



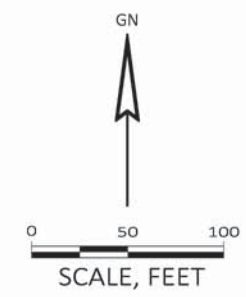
FOUND BERNSTEN MONUMENT
Y = 579,185.023
X = 870,699.649

TOWN

COURSE	BEARING	DISTANCE
200-201	N40°29'22"E	93.40'
201-202	N51°23'38"E	102.08'
202-203	N45°01'45"E	470.75'
204-205	S41°21'13"E	21.10'
205-206	S83°02'46"W	74.45'
206-207	S45°01'45"W	436.24'
207-208	SEE CURVE TABLE C1	

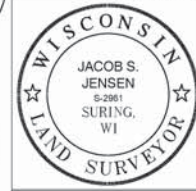
POINT	STATION	OFFSET
200	69+53.01	42.91'LT
201	70+44.91	46.54'LT
202	71+45.28	34.33'LT
203	76+16.02	33.85'LT
204	76+15.98	56.94'RT
205	76+14.62	78.00'RT
206	75+56.02	32.09'RT
207	71+19.78	31.64'RT
208	69+75.03	31.30'RT
209	68+67.17	31.06'RT

CURVE 2
PI STA = 78+46.73
Y = 577138.880
X = 871323.999
DELTA = 3°44'09" RT
D = 0°50'00"
T = 224.23'
L = 448.30'
R = 6875.49'
PC STA = 76+22.50
Y = 576980.245
X = 871165.526
PT STA = 80+70.80
Y = 577286.853
X = 871492.472
DB = N44°58'15"E
DA = N48°42'24"E



R/W CURVE TABLE				
CURVE	LENGTH	RADIUS	LONG CHORD	LONG CHORD BEARING
C1	143.25'	2831.79'	143.23'	S43°49'09"W
C2	106.69'	2831.79'	106.69'	S41°17'26"W

NOTES:
POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), OUTAGAMIE COUNTY, NAD83(2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.
ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 1" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.
ALL EXISTING IRON PINS ARE 1" I.D. IRON PIPES UNLESS OTHERWISE NOTED.
FOR THE LATEST ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN GREEN BAY.
EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE:
STH 96: RIGHT OF WAY PROJECT NUMBER 4076-3-21, RIGHT OF WAY PROJECT 4075-15-21, CSM NO. 2078, PLAT OF FIRST ADDITION TO RIVER FOREST ESTATES.
FOX MEADOWS LANE: RIGHT OF WAY PROJECT 4075-15-21, PLAT OF FIRST ADDITION TO RIVER FOREST ESTATES.
ACCESS RESTRICTIONS PER JACKET 8013, IMAGE 22, DOCUMENT NO. 925955.
FOR ADDITIONAL INFORMATION REFER TO THE TITLE SHEET, RECORDED AS SHEET 2 OF 2 OF TRANSPORTATION PROJECT PLATS, AS DOCUMENT NO. 2254732.



I, JACOB S. JENSEN, PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION, I HAVE SURVEYED AND MAPPED THIS TRANSPORTATION PROJECT PLAT 4075-35-21-4.02 AND THAT SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.
Jacob S. Jensen
JACOB S. JENSEN
REGISTRATION NUMBER: 5-2961
DECEMBER 16, 2021
DATE



THIS PLAT AND RELOCATION ORDER ARE APPROVED FOR THE WISCONSIN DEPARTMENT OF TRANSPORTATION, NORTHEAST REGION.
Curt Van Erem
CURT VAN EREM
DECEMBER 16, 2021
DATE

TRANSPORTATION PROJECT PLAT NO: 4075-35-21-4.03 AMENDMENT NO:2

ADDS PARCEL NO: 5 TO TRANSPORTATION PROJECT PLAT NO: 4075-35-21-4.03 RECORDED AS DOCUMENT NUMBER 2257068.

THAT PART OF GOVERNMENT LOT 1 OF SECTION 5 AND THAT PART OF THE NORTHWEST 1/4 OF THE SOUTHWEST 1/4 OF SECTION 4, TOWNSHIP 21 NORTH, RANGE 19 EAST, IN THE TOWN OF KAUKAUNA, OUTAGAMIE COUNTY, WISCONSIN.

RELOCATION ORDER STH 96, KAUKAUNA - WRIGHTSTOWN, CLARIBEL STREET - CTH D - OUTAGAMIE COUNTY - BROWN COUNTY

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE PROJECT.

- TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SECTION 84.02 (3), 84.09, AND 84.30, WISCONSIN STATUTES, THE DEPARTMENT OF TRANSPORTATION HEREBY ORDERS THAT:
- THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS LAID OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE PROJECT.
 - THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE DEPARTMENT FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE STATE OF WISCONSIN, PURSUANT TO THE PROVISIONS OF SECTION 84.09 (1) OR (2), WISCONSIN STATUTES.

NOTES:
POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), OUTAGAMIE COUNTY, NAD83(2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 1" X 24" IRON PIPES), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

A PERMANENT LIMITED EASEMENT (PLE) IS A RIGHT FOR CONSTRUCTION AND MAINTENANCE PURPOSES, AS DEFINED HEREIN, INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON AND THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM DESIRABLE, BUT WITHOUT PREJUDICE TO THE OWNER'S RIGHTS TO MAKE OR CONSTRUCT IMPROVEMENTS ON SAID LANDS OR TO FLATTEN THE SLOPES, PROVIDING SAID ACTIVITIES WILL NOT IMPAIR OR OTHERWISE ADVERSELY AFFECT THE HIGHWAY FACILITIES.

ALL EXISTING IRON PINS ARE 1" I.D. IRON PIPES UNLESS OTHERWISE NOTED.

FOR THE LATEST ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN GREEN BAY.

EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE:
STH 96: DIVISION JOB NUMBER 3754, CSM NO. 3999.

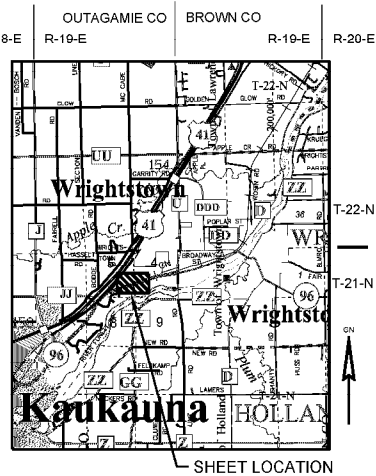
FOR ADDITIONAL INFORMATION REFER TO THE TITLE SHEET, RECORDED AS SHEET 2 OF 2 OF TRANSPORTATION PROJECT PLATS, AS DOCUMENT NO. 2254732.

PARCEL NUMBER	OWNER(S)	INTEREST(S) REQUIRED	R/W SQUARE FEET				
			FEE	EXISTING	TOTAL	TLE	PLE
4	WISCONSIN PUBLIC SERVICE CORP	TLE	-----	-----	-----	1,040	-----
5	WISCONSIN BELL, INC.	FEE	3,560	-----	3,560	-----	-----
6	JANICE M. VANDE HEY IRREVOCABLE TRUST	FEE, TLE	875	-----	875	264	-----
7	NORMAN ROLAND BENEDICT	PLE, TLE	-----	-----	-----	1,324	4,222

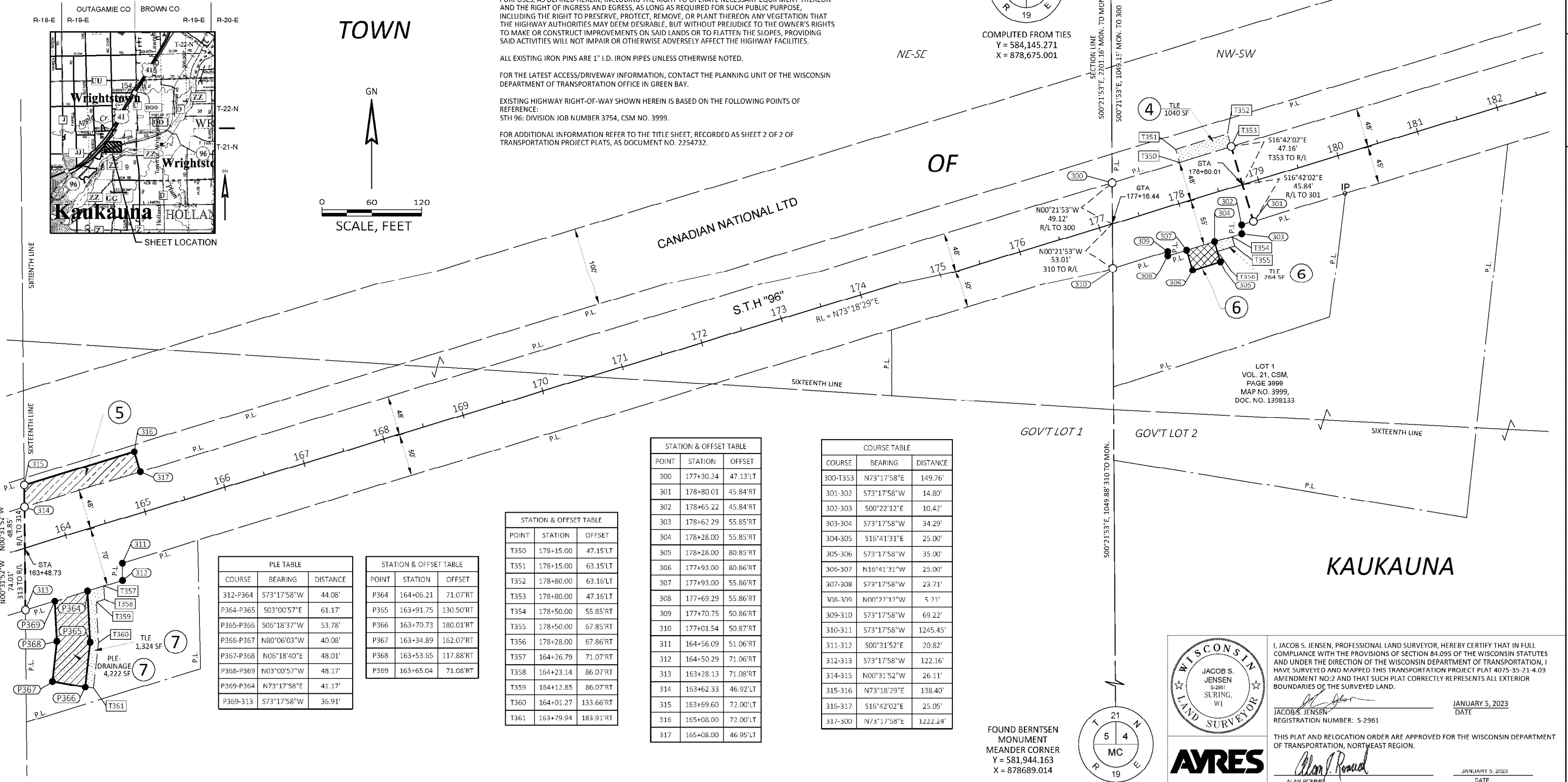
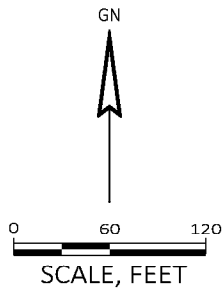
OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE DEPARTMENT.
PURPOSE OF ALL THE TLE'S ARE FOR GRADING PURPOSES, UNLESS NOTED.

Document #: 2285981
Date: 01-06-2023 Time: 10:19 AM
Pages: 1 Fee: \$25.00
County: OUTAGAMIE COUNTY State: WI
Sarah R. Van Camp
SARAH R VAN CAMP, REGISTER OF DEEDS
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RESERVED FOR REGISTER OF DEEDS
PROJECT NUMBER 4075-35-21-4.03
AMENDMENT NO:2



TOWN



POINT	STATION	OFFSET
300	177+30.24	47.13'LT
301	178+80.01	45.84'RT
302	178+65.22	45.84'RT
303	178+62.29	55.85'RT
304	178+28.00	55.85'RT
305	178+28.00	80.85'RT
306	177+93.00	80.86'RT
307	177+93.00	55.86'RT
308	177+69.29	55.86'RT
309	177+70.75	50.86'RT
310	177+01.54	50.87'RT
311	164+56.09	51.06'RT
312	164+50.29	71.06'RT
313	163+28.13	71.08'RT
314	163+62.33	46.92'LT
315	163+69.60	72.00'LT
316	165+08.00	72.00'LT
317	165+08.00	46.95'LT

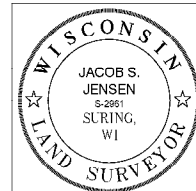
COURSE	BEARING	DISTANCE
300-T353	N73°17'58"E	149.76'
301-302	S73°17'58"W	14.80'
302-303	S00°22'12"E	10.42'
303-304	S73°17'58"W	34.29'
304-305	S16°41'31"E	25.00'
305-306	S73°17'58"W	35.00'
306-307	N16°41'31"W	25.00'
307-308	S73°17'58"W	23.71'
308-309	N00°22'12"W	5.71'
309-310	S73°17'58"W	69.22'
310-311	S73°17'58"W	1245.45'
311-312	S00°31'52"E	20.82'
312-313	S73°17'58"W	122.16'
314-315	N00°31'52"W	26.11'
315-316	N73°18'29"E	138.40'
316-317	S16°42'02"E	25.05'
317-300	N73°17'58"E	1222.24'

POINT	STATION	OFFSET
T350	178+15.00	47.15'LT
T351	178+15.00	63.15'LT
T352	178+80.00	63.16'LT
T353	178+80.00	47.16'LT
T354	178+50.00	55.85'RT
T355	178+50.00	67.85'RT
T356	178+28.00	67.86'RT
T357	164+26.79	71.07'RT
T358	164+23.14	86.07'RT
T359	164+12.85	86.07'RT
T360	164+01.27	133.66'RT
T361	163+79.94	183.91'RT

COURSE	BEARING	DISTANCE
312-P364	S73°17'58"W	44.08'
P364-P365	S03°00'57"E	61.17'
P365-P366	S06°18'37"W	53.78'
P366-P367	N80°06'03"W	40.08'
P367-P368	N06°18'40"E	48.01'
P368-P369	N03°00'57"W	48.17'
P369-P364	N73°17'58"E	41.17'
P369-313	S73°17'58"W	36.91'

POINT	STATION	OFFSET
P364	164+06.21	71.07'RT
P365	163+91.75	130.50'RT
P366	163+70.73	180.01'RT
P367	163+34.89	162.07'RT
P368	163+53.65	117.88'RT
P369	163+65.04	71.08'RT

FOUND BERNTSEN MONUMENT MEANDER CORNER
Y = 581,944.163
X = 878689.014



I, JACOB S. JENSEN, PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION, I HAVE SURVEYED AND MAPPED THIS TRANSPORTATION PROJECT PLAT 4075-35-21-4.03 AMENDMENT NO:2 AND THAT SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.
Jacob S. Jensen
JACOB S. JENSEN
REGISTRATION NUMBER: 5-2961
DATE: JANUARY 5, 2023
THIS PLAT AND RELOCATION ORDER ARE APPROVED FOR THE WISCONSIN DEPARTMENT OF TRANSPORTATION, NORTHEAST REGION.
Alan Rommel
ALAN ROMMEL
DATE: JANUARY 5, 2023

TRANSPORTATION PROJECT PLAT NO: 4075-35-21-4.04

THAT PART OF OUTLOT 19 OF ASSESSOR'S PLAT AND REPLAT OF THE VILLAGE OF WRIGHTSTOWN, LOCATED IN THE SOUTHWEST 1/4 OF THE NORTHWEST 1/4 OF SECTION 3, TOWNSHIP 21 NORTH, RANGE 19 EAST, IN THE VILLAGE OF WRIGHTSTOWN, BROWN COUNTY, WISCONSIN.

RELOCATION ORDER STH 96, KAUKAUNA - WRIGHTSTOWN, CLARIBEL STREET - CTH D - OUTAGAMIE COUNTY - BROWN COUNTY

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE PROJECT.

TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SECTION 84.02 (3), 84.09, AND 84.30, WISCONSIN STATUTES, THE DEPARTMENT OF TRANSPORTATION HEREBY ORDERS THAT:

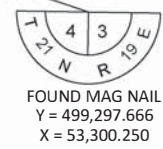
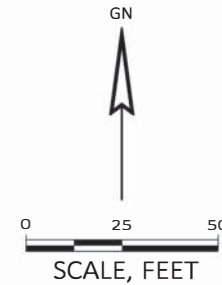
- THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS LAID OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE PROJECT.
- THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE DEPARTMENT FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE STATE OF WISCONSIN, PURSUANT TO THE PROVISIONS OF SECTION 84.09 (1) OR (2), WISCONSIN STATUTES.

SCHEDULE OF LANDS & INTERESTS REQUIRED						
PARCEL NUMBER	OWNER(S)	INTEREST REQUIRED	R/W SQUARE FEET			TLE ACRES
			NEW	EXISTING	TOTAL	
8	RICHARD J. BASTIAN	FEE	350	-----	350	-----

OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE DEPARTMENT.
PURPOSE OF ALL THE TLE'S ARE FOR GRADING PURPOSES, UNLESS NOTED.

2985516
VOLUME:
PAGE:
CHERYL BERKEN
BROWN COUNTY
REGISTER OF DEEDS
GREEN BAY, WI
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11/22/2021 09:49 AM
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RESERVED FOR REGISTER OF DEEDS
PROJECT NUMBER 4075-35-21-4.04
SHEET 1 OF 2



FOUND MAG NAIL
Y = 499,297.666
X = 53,300.250

NOTES:
POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), BROWN COUNTY, NAD83(2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 1" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

ALL EXISTING IRON PINS ARE 1" I.D. IRON PIPES UNLESS OTHERWISE NOTED.

FOR THE LATEST ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN GREEN BAY.

EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE:
STH 96/MAIN STREET: DIVISION JOB NO. 3754, RIGHT OF WAY PROJECT NO. 4095-05-21, ASSESSOR'S PLAT & REPLAT OF THE VILLAGE OF WRIGHTSTOWN.

FOR ADDITIONAL INFORMATION REFER TO THE TITLE SHEET, RECORDED AS SHEET 2 OF 2.

VILLAGE

OUTLOT 18

S.T.H "96" MAIN STREET

OF

OUTLOT 19

PLAT OF SURVEY
21-19-3-003

WRIGHTSTOWN

OUTLOT 20

COURSE TABLE		
COURSE	BEARING	DISTANCE
401-402	N79°11'38"E	197.31'
402-403	S88°37'13"E	33.73'
404-405	S79°11'38"W	22.27'
405-406	S10°50'28"E	14.00'
406-407	S79°11'38"W	25.00'
407-408	N10°50'28"W	14.00'
408-400	S79°11'38"W	183.00'

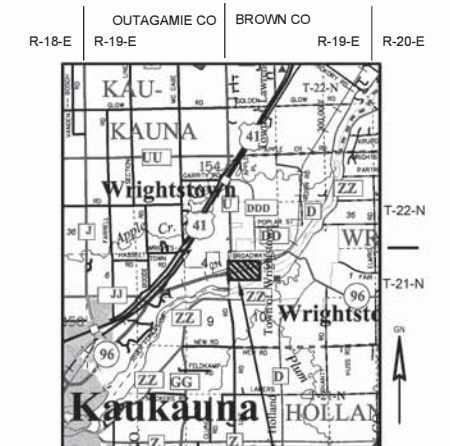
STATION & OFFSET TABLE		
POINT	STATION	OFFSET
400	233+00.00	40.56'RT
401	233+00.00	46.56'LT
402	234+97.31	46.44'LT
403	235+30.28	39.30'LT
404	235+30.28	40.70'RT
405	235+08.00	40.69'RT
406	235+08.00	54.69'RT
407	234+83.00	54.67'RT
408	234+83.00	40.67'RT

ASSESSOR'S PLAT & REPLAT OF THE VILLAGE OF WRIGHTSTOWN

50 AT&T
NO EASEMENT OF RECORD
- PARCEL 8

UTILITY INTERESTS REQUIRED

UTILITY NO.	OWNER(S)	INTEREST REQUIRED
50	AT&T	RELEASE OF RIGHTS



SHEET LOCATION

FOUND HARRISON MONUMENT
Y = 496,649.387
X = 53,283.829

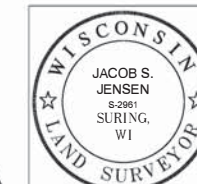
See Affidavit of Correction Doc No. 2987344 for correct distance.

S88°25'08"E 1979.64'
MON TO TRAV. PNT.

S88°25'08"E 3373.32'
TRAV.PNT.TO MON

QUARTER LINE
S88°25'08"E 5352.96' MON TO MON

FOUND BRASS CAP MONUMENT
Y = 496,501.691
X = 58,634.751



I, JACOB S. JENSEN, PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION, I HAVE SURVEYED AND MAPPED THIS TRANSPORTATION PROJECT PLAT 4075-35-21-4.04 AND THAT SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED ... LAND.

JACOB S. JENSEN
REGISTRATION NUMBER: 5-2961

NOVEMBER 19, 2021
DATE

THIS PLAT AND RELOCATION ORDER ARE APPROVED FOR THE WISCONSIN DEPARTMENT OF TRANSPORTATION, NORTHEAST REGION.

Curt Van Erem
CURT VAN EREM

NOVEMBER 19, 2021
DATE

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION TRANSPORTATION PROJECT PLAT TITLE SHEET

4075-35-21

KAUKAUNA - WRIGHTSTOWN

CLARIBEL STREET - CTH D

STH 96

BROWN COUNTY - OUTAGAMIE COUNTY



PROJECT LOCATION

CONVENTIONAL SYMBOLS

SECTION LINE	---	SECTION CORNER SYMBOL		R/W MONUMENT (TO BE SET)	●
QUARTER LINE	---	SECTION CORNER MONUMENT		NON-MONUMENTED R/W POINT	○
SIXTEENTH LINE	---	GEODETIC SURVEY MONUMENT		FOUND IRON PIN (1-INCH UNLESS NOTED)	!P
NEW REFERENCE LINE	---	SIXTEENTH CORNER MONUMENT		OFF-PREMISE SIGN	
NEW R/W LINE	---	SIGN		COMPENSABLE	
EXISTING R/W OR HE LINE	---	NON-COMPENSABLE		NO ACCESS (BY STATUTORY AUTHORITY)	
PROPERTY LINE	---	ACCESS RESTRICTED (BY PREVIOUS PROJECT OR CONTROL)		NO ACCESS (NEW HIGHWAY)	
LOT, TIE & OTHER MINOR LINES	---	NO ACCESS (NEW HIGHWAY)		PARCEL NUMBER	
SLOPE INTERCEPT	---	PARCEL NUMBER		UTILITY NUMBER	
CORPORATE LIMITS	---	UTILITY NUMBER		PARALLEL OFFSETS	
UNDERGROUND FACILITY (COMMUNICATIONS, ELECTRIC, ETC.)	---	PARALLEL OFFSETS			
NEW R/W (FEE OR HE) (HATCHING VARIES BY OWNER)	---				
TEMPORARY LIMITED EASEMENT AREA	---				
EASEMENT AREA (PERMANENT LIMITED OR RESTRICTED DEVELOPMENT)	---				
TRANSMISSION STRUCTURES	---				
BUILDING		TO BE REMOVED			
BRIDGE					

CONVENTIONAL ABBREVIATIONS

ACCESS RIGHTS	AR	POINT OF INTERSECTION	PI
ACRES	AC	PROPERTY LINE	PL
AHEAD	AH	RECORDED AS	(100')
ALUMINUM	ALUM	REEL / IMAGE	R/I
AND OTHERS	ET AL	REFERENCE LINE	R/L
BACK	BK	REMAINING	REM
BLOCK	BLK	RESTRICTIVE DEVELOPMENT	RDE
CENTERLINE	C/L	EASEMENT	
CERTIFIED SURVEY MAP	CSM	RIGHT	RT
CONCRETE	CONC	RIGHT OF WAY	R/W
COUNTY	CO	SECTION	SEC
COUNTY TRUNK HIGHWAY	CTH	SEPTIC VENT	SEPV
DISTANCE	DIST	SQUARE FEET	SF
CORNER	COR	STATE TRUNK HIGHWAY	STH
DOCUMENT NUMBER	DOC	STATION	STA
EASEMENT	EASE	TELEPHONE PEDESTAL	TP
EXISTING	EX	TEMPORARY LIMITED	TLE
GAS VALVE	GV	EASEMENT	
GRID NORTH	GN	TRANSPORTATION PROJECT	TPP
HIGHWAY EASEMENT	HE	PLAT	
IDENTIFICATION	ID	UNITED STATES HIGHWAY	USH
LAND CONTRACT	LC	VOLUME	V
LEFT	LT		
MONUMENT	MON		
NATIONAL GEODETIC SURVEY	NGS		
NUMBER	NO		
OUTLOT	OL		
PAGE	P		
POINT OF TANGENCY	PT		
PERMANENT LIMITED EASEMENT	PLE		
POINT OF BEGINNING	POB		
POINT OF CURVATURE	PC		
POINT OF COMPOUND CURVE	PCC		

CURVE DATA

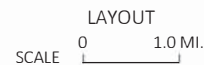
LONG CHORD	LCH
LONG CHORD BEARING	LCB
RADIUS	R
DEGREE OF CURVE	D
CENTRAL ANGLE	Δ/DELTA
LENGTH OF CURVE	L
TANGENT	T
DIRECTION AHEAD	DA
DIRECTION BACK	DB

CONVENTIONAL UTILITY SYMBOLS

W	WATER
G	GAS
T	TELEPHONE
OH	OVERHEAD TRANSMISSION LINES
E	ELECTRIC
TV	CABLE TELEVISION
FO	FIBER OPTIC
SA	SANITARY SEWER
SS	STORM SEWER



PROJECT LOCATION



THE NOTES, CONVENTIONAL SIGNS, AND ABBREVIATIONS ARE ASSOCIATED WITH EACH TRANSPORTATION PROJECT PLAT FOR PROJECT 4075-35-21.

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), BROWN COUNTY, NAD83(2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 1" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

ALL RIGHT-OF-WAY LINES DEPICTED IN THE NON-ACQUISITION AREAS ARE INTENDED TO RE-ESTABLISH EXISTING RIGHT-OF-WAY LINES AS DETERMINED FROM PREVIOUS PROJECTS, OTHER RECORDED DOCUMENTS, OR FROM CENTERLINE OF EXISTING PAVEMENTS.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS" OF PUBLIC RECORD.

DIMENSIONING FOR THE NEW RIGHT-OF-WAY IS MEASURED ALONG AND PERPENDICULAR TO THE NEW REFERENCE LINES.

A TEMPORARY LIMITED EASEMENT (TLE) IS A RIGHT FOR CONSTRUCTION PURPOSES, AS DEFINED HEREIN, INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON, THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM DESIRABLE. ALL (TLEs) ON THIS PLAT EXPIRE AT THE COMPLETION OF THE CONSTRUCTION PROJECT FOR WHICH THIS INSTRUMENT IS GIVEN.

PROPERTY LINES SHOWN ON THIS PLAT ARE DRAWN FROM DATA DERIVED FROM MAPS AND DOCUMENTS OF PUBLIC RECORD AND/OR EXISTING OCCUPATIONAL LINES. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.

FOR THE LATEST ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN GREEN BAY.

PARCEL AND IDENTIFICATION NUMBERS MAY NOT POINT TO ALL AREAS OF ACQUISITION, AS NOTED ON THE TPP DETAIL PAGES.

INFORMATION FOR THE BASIS OF EXISTING HIGHWAY RIGHT-OF-WAY POINTS OF REFERENCE AND ACCESS CONTROL ARE LISTED ON THE TPP DETAIL PAGES.

PROJECT NUMBER 4075-35-21 -4.04
SHEET 2 OF 2
AMENDMENT NO:

4

4

TRANSPORTATION PROJECT PLAT NO: 4075-35-21-4.05

THAT PART OF OUTLOTS 5 AND 6, OF ASSESSOR'S PLAT AND REPLAT OF THE VILLAGE OF WRIGHTSTOWN, AND THAT PART OF LOT 6, BLOCK 1, AND THAT PART OF LOTS 1 AND 5, BLOCK 2 OF ASSESSOR'S PLAT AND REPLAT OF CLARK'S ADDITION, LOCATED IN PART OF GOVERNMENT LOT 7 OF SECTION 3, TOWNSHIP 21 NORTH, RANGE 19 EAST, IN THE VILLAGE OF WRIGHTSTOWN, BROWN COUNTY, WISCONSIN.

RELOCATION ORDER STH 96, KAUKAUNA - WRIGHTSTOWN, CLARIBEL STREET - CTH D - OUTAGAMIE COUNTY - BROWN COUNTY

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE PROJECT.

TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SECTION 84.02 (3), 84.09, AND 84.30, WISCONSIN STATUTES, THE DEPARTMENT OF TRANSPORTATION HEREBY ORDERS THAT:

- THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS LAID OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE PROJECT.
- THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE DEPARTMENT FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE STATE OF WISCONSIN, PURSUANT TO THE PROVISIONS OF SECTION 84.09 (1) OR (2), WISCONSIN STATUTES.

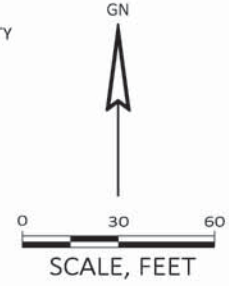
PARCEL NUMBER	OWNER(S)	INTEREST(S) REQUIRED	R/W SQUARE FEET			
			NEW	EXISTING	TOTAL	TLE
11	URBAN KLISTER POST NO. 436, OF THE AMERICAN LEGION, DEPARTMENT OF WISCONSIN	FEE & TLE	249	-----	249	256
12	JEFFREY J. AND MARY K. BERMAN	TLE	-----	-----	-----	285
13	THEODORE E. AND SALLEY A. COENEN	TLE	-----	-----	-----	275
14	NICHOLE M. HOOD	TLE	-----	-----	-----	288
16	DOROTHY LAMERS	TLE	-----	-----	-----	262
17	LINDA H. LIESENER ETAL	FEE & TLE	53	-----	53	415

OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE DEPARTMENT. PURPOSE OF ALL THE TLE'S ARE FOR GRADING PURPOSES, UNLESS NOTED.

FOUND MAG NAIL WITH WASHER
Y = 499,281.724
X = 58,646.571

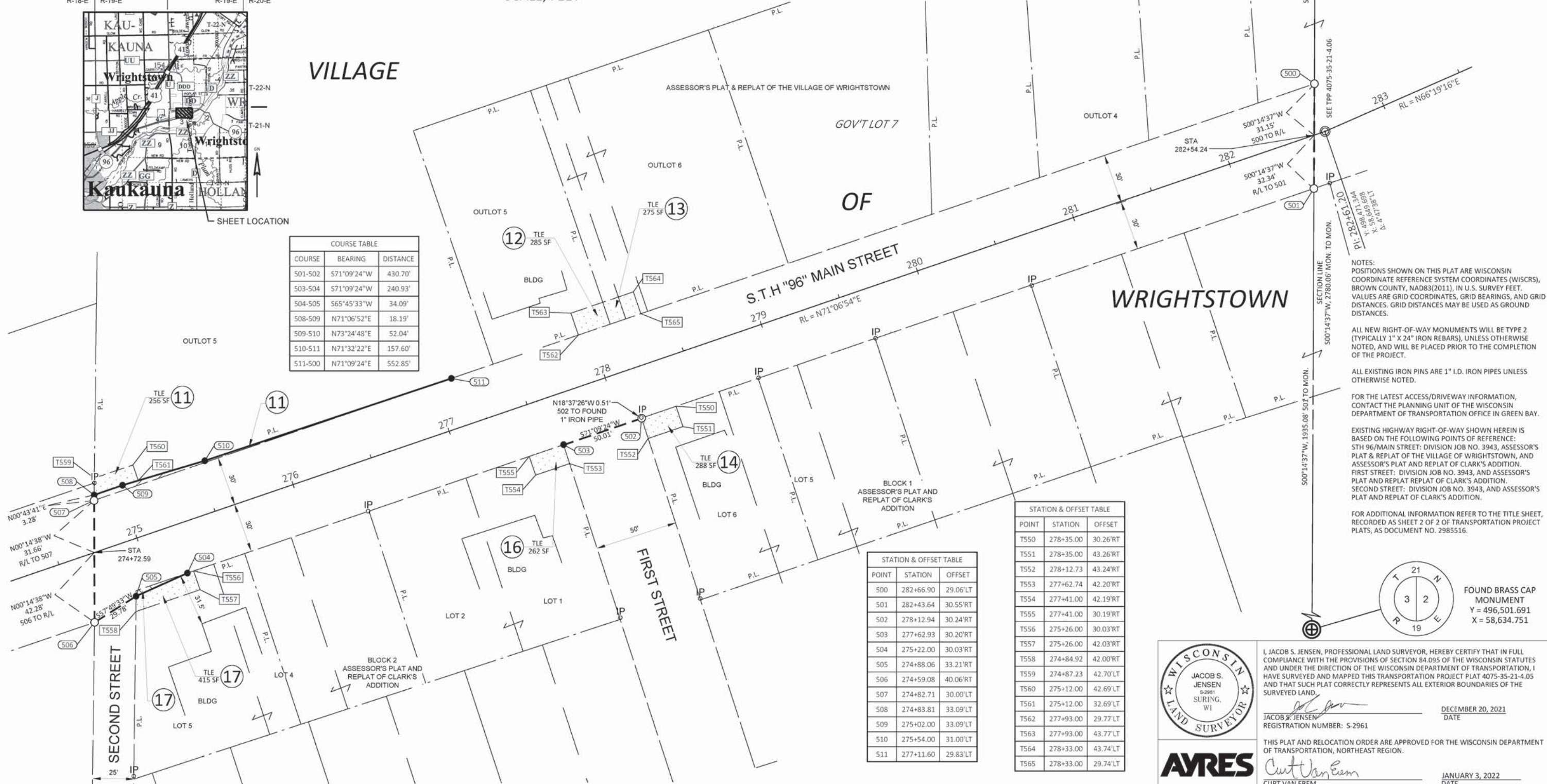
2990362
VOLUME:
PAGE:
CHERYL BERKEN
BROWN COUNTY
REGISTER OF DEEDS
GREEN BAY, WI
RECORDED ON
01/03/2022 12:17 PM
REC FEE: 25.00
PAGES: 1
*The above recording information verifies that this document has been electronically recorded and returned to the submitter**

RESERVED FOR REGISTER OF DEEDS
PROJECT NUMBER 4075-35-21-4.05
SHEET 1 OF 1



VILLAGE

COURSE	BEARING	DISTANCE
501-502	S71°09'24"W	430.70'
503-504	S71°09'24"W	240.93'
504-505	S65°45'33"W	34.09'
508-509	N71°06'52"E	18.19'
509-510	N73°24'48"E	52.04'
510-511	N71°32'22"E	157.60'
511-500	N71°09'24"E	552.85'



POINT	STATION	OFFSET
T550	278+35.00	30.26'RT
T551	278+35.00	43.26'RT
T552	278+12.73	43.24'RT
T553	277+62.74	42.20'RT
T554	277+41.00	42.19'RT
T555	277+41.00	30.19'RT
T556	275+26.00	30.03'RT
T557	275+26.00	42.03'RT
T558	274+84.92	42.00'RT
T559	274+87.23	42.70'LT
T560	275+12.00	42.69'LT
T561	275+12.00	32.69'LT
T562	277+93.00	29.77'LT
T563	277+93.00	43.77'LT
T564	278+33.00	43.74'LT
T565	278+33.00	29.74'LT

POINT	STATION	OFFSET
500	282+66.90	29.06'LT
501	282+43.64	30.55'RT
502	278+12.94	30.24'RT
503	277+62.93	30.20'RT
504	275+22.00	30.03'RT
505	274+88.06	33.21'RT
506	274+59.08	40.06'RT
507	274+82.71	30.00'LT
508	274+83.81	33.09'LT
509	275+02.00	33.09'LT
510	275+54.00	31.00'LT
511	277+11.60	29.83'LT

NOTES:
POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), BROWN COUNTY, NAD83(2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

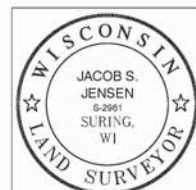
ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 1" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

ALL EXISTING IRON PINS ARE 1" I.D. IRON PIPES UNLESS OTHERWISE NOTED.

FOR THE LATEST ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN GREEN BAY.

EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE:
STH 96/MAIN STREET: DIVISION JOB NO. 3943, ASSESSOR'S PLAT & REPLAT OF THE VILLAGE OF WRIGHTSTOWN, AND ASSESSOR'S PLAT AND REPLAT OF CLARK'S ADDITION.
FIRST STREET: DIVISION JOB NO. 3943, AND ASSESSOR'S PLAT AND REPLAT OF CLARK'S ADDITION.
SECOND STREET: DIVISION JOB NO. 3943, AND ASSESSOR'S PLAT AND REPLAT OF CLARK'S ADDITION.

FOR ADDITIONAL INFORMATION REFER TO THE TITLE SHEET, RECORDED AS SHEET 2 OF 2 OF TRANSPORTATION PROJECT PLATS, AS DOCUMENT NO. 2985516.



I, JACOB S. JENSEN, PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION, I HAVE SURVEYED AND MAPPED THIS TRANSPORTATION PROJECT PLAT 4075-35-21-4.05 AND THAT SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.

JACOB S. JENSEN
REGISTRATION NUMBER: S-2961

DECEMBER 20, 2021
DATE



THIS PLAT AND RELOCATION ORDER ARE APPROVED FOR THE WISCONSIN DEPARTMENT OF TRANSPORTATION, NORTHEAST REGION.

Curt Van Erem
CURT VAN EREM

JANUARY 3, 2022
DATE

TRANSPORTATION PROJECT PLAT NO: 4075-35-21-4.06

THAT PART OF OUTLOT 2 OF THE ASSESSOR'S PLAT AND REPLAT OF GOVERNMENT LOT 7 OF SECTION 3, PART OF LOT 2, BLOCK 8, PART OF LOTS 5, 6, 8-12, BLOCK 7, OF ASSESSOR'S PLAT AND REPLAT OF THE VILLAGE OF WRIGHTSTOWN, AND THAT PART OF LOTS 1 AND 2, BLOCK 9, OF ASSESSOR'S PLAT AND REPLAT OF JOHNSON'S SUBDIVISION, AND PART OF LOTS 1, 18-19, AND OUTLOT 1, BLOCK 9 OF ASSESSOR'S PLAT AND REPLAT OF CLOUGH'S SUBDIVISION, LOCATED IN PART OF GOVERNMENT LOT 4 OF SECTION 2, ALL LOCATED IN TOWNSHIP 21 NORTH, RANGE 19 EAST, IN THE VILLAGE OF WRIGHTSTOWN, BROWN COUNTY, WISCONSIN.

RELOCATION ORDER STH 96, KAUKAUNA - WRIGHTSTOWN, CLARIBEL STREET - CTH D - OUTAGAMIE COUNTY - BROWN COUNTY

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE PROJECT.

TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SECTION 84.02 (3), 84.09, AND 84.30, WISCONSIN STATUTES, THE DEPARTMENT OF TRANSPORTATION HEREBY ORDERS THAT:

- THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS LAID OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE PROJECT.
- THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE DEPARTMENT FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE STATE OF WISCONSIN, PURSUANT TO THE PROVISIONS OF SECTION 84.09 (1) OR (2), WISCONSIN STATUTES.

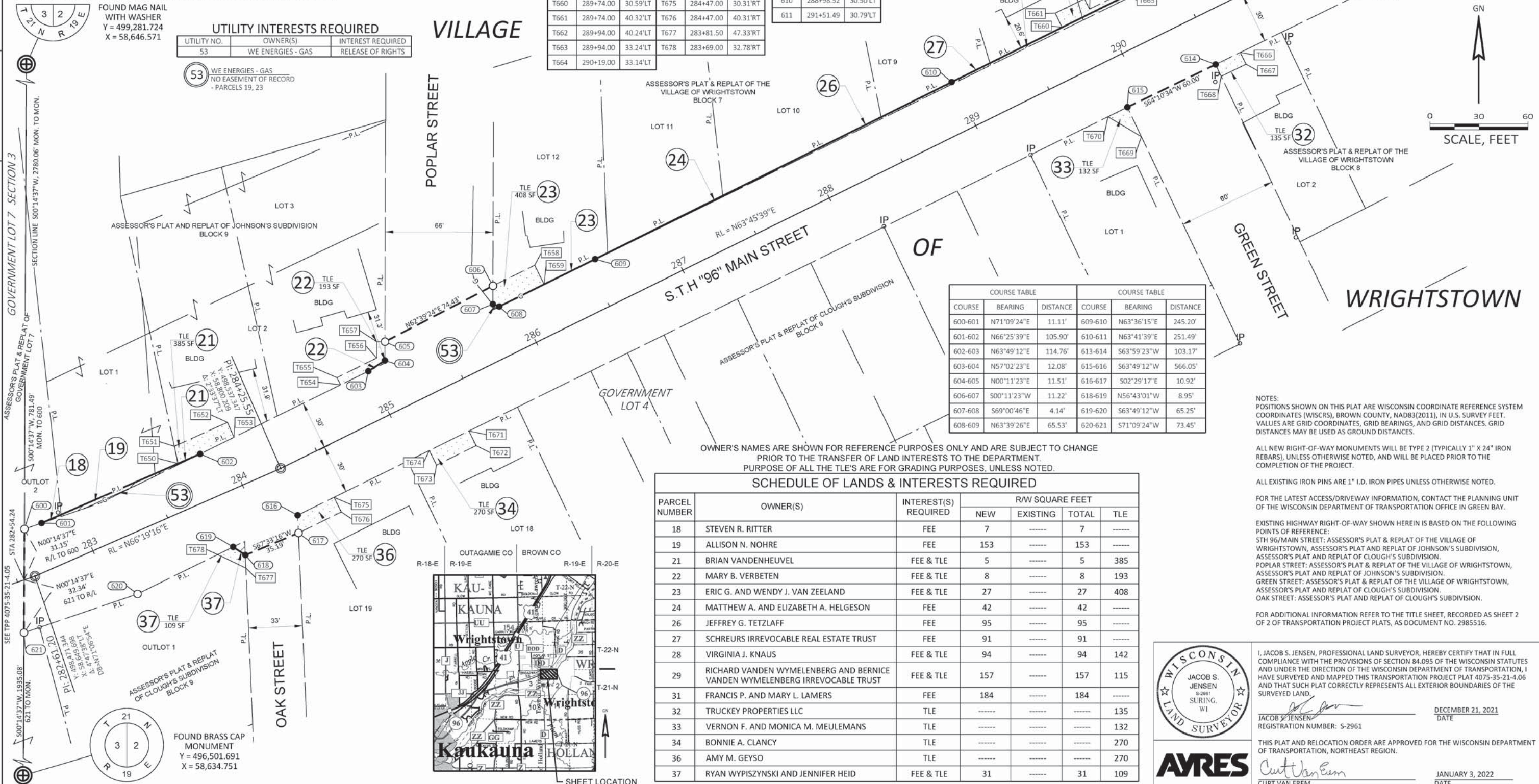
FOUND MAG NAIL WITH WASHER
Y = 499,281.724
X = 58,646.571

UTILITY INTERESTS REQUIRED

UTILITY NO.	OWNER(S)	INTEREST REQUIRED
53	WE ENERGIES - GAS	RELEASE OF RIGHTS

53 WE ENERGIES - GAS
NO EASEMENT OF RECORD
- PARCELS 19, 23

STATION & OFFSET TABLE			STATION & OFFSET TABLE			STATION & OFFSET TABLE			STATION & OFFSET TABLE		
POINT	STATION	OFFSET	POINT	STATION	OFFSET	POINT	STATION	OFFSET	POINT	STATION	OFFSET
T650	283+69.17	27.95'LT	T665	290+19.00	30.64'LT	600	282+66.90	29.06'LT	612	291+51.39	28.29'LT
T651	283+71.33	38.53'LT	T666	290+65.00	31.36'RT	601	282+77.97	28.12'LT	613	291+51.39	31.71'RT
T652	284+07.00	38.93'LT	T667	290+65.00	39.36'RT	602	283+83.86	27.92'LT	614	290+48.23	31.30'RT
T653	284+07.00	28.93'LT	T668	290+48.12	39.30'RT	603	284+99.92	29.64'LT	615	289+88.23	30.86'RT
T654	284+81.00	29.66'LT	T669	289+87.95	50.85'RT	604	285+11.91	31.05'LT	616	284+23.53	30.40'RT
T655	284+81.00	34.66'LT	T670	289+75.00	30.85'RT	605	285+17.03	41.36'LT	617	284+19.58	40.58'RT
T656	285+08.10	34.63'LT	T671	285+39.00	30.40'RT	606	285+91.45	42.80'LT	618	283+84.40	39.82'RT
T657	285+12.56	43.59'LT	T672	285+39.00	40.40'RT	607	285+86.46	32.75'LT	619	283+79.52	32.32'RT
T658	286+20.00	42.77'LT	T673	285+12.00	40.37'RT	608	285+89.27	29.71'LT	620	283+14.34	35.17'RT
T659	286+20.00	29.77'LT	T674	285+12.00	30.37'RT	609	286+54.80	29.83'LT	621	282+43.64	30.55'RT
T660	289+74.00	30.59'LT	T675	284+47.00	30.31'RT	610	288+98.52	30.50'LT			
T661	289+74.00	40.32'LT	T676	284+47.00	40.31'RT	611	291+51.49	30.79'LT			
T662	289+94.00	40.24'LT	T677	283+81.50	47.33'RT						
T663	289+94.00	33.24'LT	T678	283+69.00	32.78'RT						
T664	290+19.00	33.14'LT									



2990363
VOLUME:
PAGE:
CHERYL BERKEN
BROWN COUNTY
REGISTER OF DEEDS
GREEN BAY, WI
RECORDED ON
01/03/2022 12:19 PM
REC FEE: 25.00
PAGES: 1
*The above recording information
verifies that this document has
been electronically recorded and
returned to the submitter**

RESERVED FOR REGISTER OF DEEDS
PROJECT NUMBER 4075-35-21-4.06
SHEET 1 OF 1

COURSE TABLE

COURSE	BEARING	DISTANCE	COURSE	BEARING	DISTANCE
600-601	N71°09'24"E	11.11'	609-610	N63°36'15"E	245.20'
601-602	N66°25'39"E	105.90'	610-611	N63°41'39"E	251.49'
602-603	N63°49'12"E	114.76'	613-614	S63°59'23"W	103.17'
603-604	N57°02'23"E	12.08'	615-616	S63°49'12"W	566.05'
604-605	N00°11'23"E	11.51'	616-617	S02°29'17"E	10.92'
606-607	S00°11'23"W	11.22'	618-619	N56°43'01"W	8.95'
607-608	S69°00'46"E	4.14'	619-620	S63°49'12"W	65.25'
608-609	N63°39'26"E	65.53'	620-621	S71°09'24"W	73.45'

OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO THE DEPARTMENT.
PURPOSE OF ALL THE TLE'S ARE FOR GRADING PURPOSES, UNLESS NOTED.

SCHEDULE OF LANDS & INTERESTS REQUIRED

PARCEL NUMBER	OWNER(S)	INTEREST(S) REQUIRED	R/W SQUARE FEET			
			NEW	EXISTING	TOTAL	TLE
18	STEVEN R. RITTER	FEE	7	-----	7	-----
19	ALLISON N. NOHRE	FEE	153	-----	153	-----
21	BRIAN VANDENHEUVEL	FEE & TLE	5	-----	5	385
22	MARY B. VERBETEN	FEE & TLE	8	-----	8	193
23	ERIC G. AND WENDY J. VAN ZEELAND	FEE & TLE	27	-----	27	408
24	MATTHEW A. AND ELIZABETH A. HELGESON	FEE	42	-----	42	-----
26	JEFFREY G. TETZLAFF	FEE	95	-----	95	-----
27	SCHREURS IRREVOCABLE REAL ESTATE TRUST	FEE	91	-----	91	-----
28	VIRGINIA J. KNAUS	FEE & TLE	94	-----	94	142
29	RICHARD VANDEN WYMELENBERG AND BERNICE VANDEN WYMELENBERG IRREVOCABLE TRUST	FEE & TLE	157	-----	157	115
31	FRANCIS P. AND MARY L. LAMERS	FEE	184	-----	184	-----
32	TRUCKEY PROPERTIES LLC	TLE	-----	-----	-----	135
33	VERNON F. AND MONICA M. MEULEMANS	TLE	-----	-----	-----	132
34	BONNIE A. CLANCY	TLE	-----	-----	-----	270
36	AMY M. GEYSO	TLE	-----	-----	-----	270
37	RYAN WYPYSZYNSKI AND JENNIFER HEID	FEE & TLE	31	-----	31	109

NOTES:
POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COORDINATE REFERENCE SYSTEM COORDINATES (WISCRS), BROWN COUNTY, NAD83(2011), IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY 1" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

ALL EXISTING IRON PINS ARE 1" I.D. IRON PIPES UNLESS OTHERWISE NOTED.

FOR THE LATEST ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN GREEN BAY.

EXISTING HIGHWAY RIGHT-OF-WAY SHOWN HEREIN IS BASED ON THE FOLLOWING POINTS OF REFERENCE:
STH 96/MAIN STREET: ASSESSOR'S PLAT & REPLAT OF THE VILLAGE OF WRIGHTSTOWN, ASSESSOR'S PLAT AND REPLAT OF JOHNSON'S SUBDIVISION, ASSESSOR'S PLAT AND REPLAT OF CLOUGH'S SUBDIVISION.
POPLAR STREET: ASSESSOR'S PLAT & REPLAT OF THE VILLAGE OF WRIGHTSTOWN, ASSESSOR'S PLAT AND REPLAT OF JOHNSON'S SUBDIVISION.
GREEN STREET: ASSESSOR'S PLAT & REPLAT OF THE VILLAGE OF WRIGHTSTOWN, ASSESSOR'S PLAT AND REPLAT OF CLOUGH'S SUBDIVISION.
OAK STREET: ASSESSOR'S PLAT AND REPLAT OF CLOUGH'S SUBDIVISION.

FOR ADDITIONAL INFORMATION REFER TO THE TITLE SHEET, RECORDED AS SHEET 2 OF 2 OF TRANSPORTATION PROJECT PLATS, AS DOCUMENT NO. 2985516.

WISCONSIN LAND SURVEYOR
JACOB S. JENSEN
20201 SURVING, WI

I, JACOB S. JENSEN, PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION, I HAVE SURVEYED AND MAPPED THIS TRANSPORTATION PROJECT PLAT 4075-35-21-4.06 AND THAT SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.

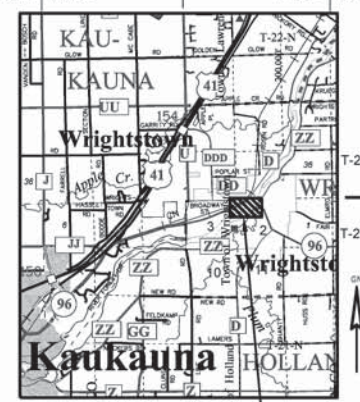
JACOB S. JENSEN
REGISTRATION NUMBER: S-2961

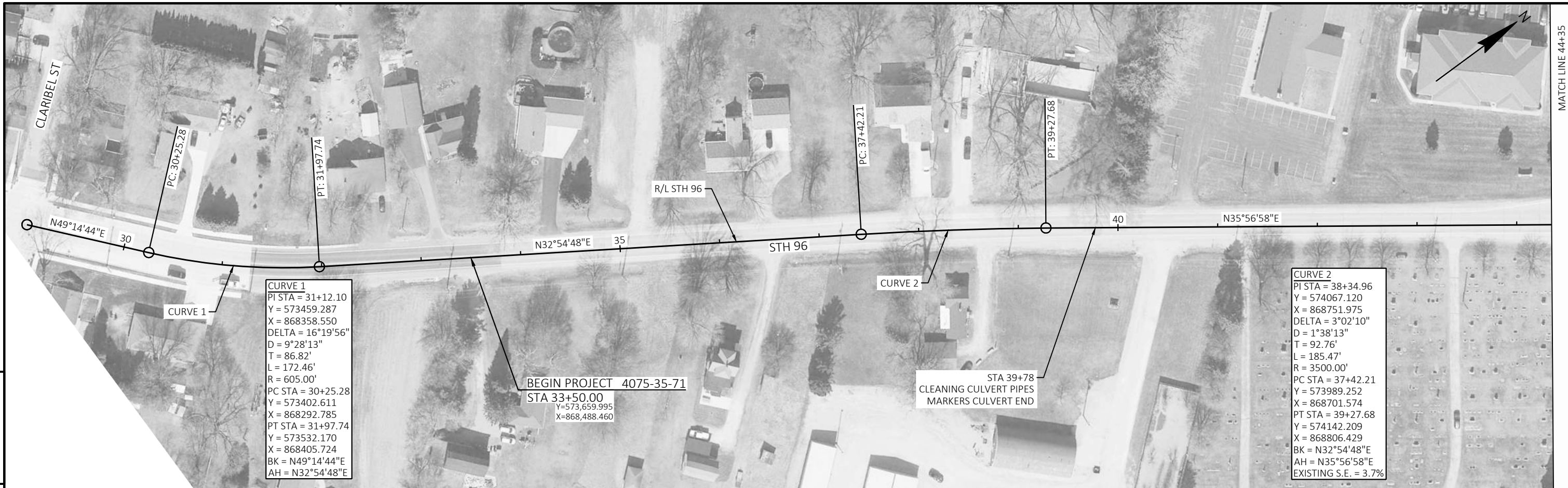
DECEMBER 21, 2021
DATE

THIS PLAT AND RELOCATION ORDER ARE APPROVED FOR THE WISCONSIN DEPARTMENT OF TRANSPORTATION, NORTHEAST REGION.

AVRES
CURT VAN EREM

JANUARY 3, 2022
DATE





CURVE 1
 PI STA = 31+12.10
 Y = 573459.287
 X = 868358.550
 DELTA = 16°19'56"
 D = 9°28'13"
 T = 86.82'
 L = 172.46'
 R = 605.00'
 PC STA = 30+25.28
 Y = 573402.611
 X = 868292.785
 PT STA = 31+97.74
 Y = 573532.170
 X = 868405.724
 BK = N49°14'44"E
 AH = N32°54'48"E

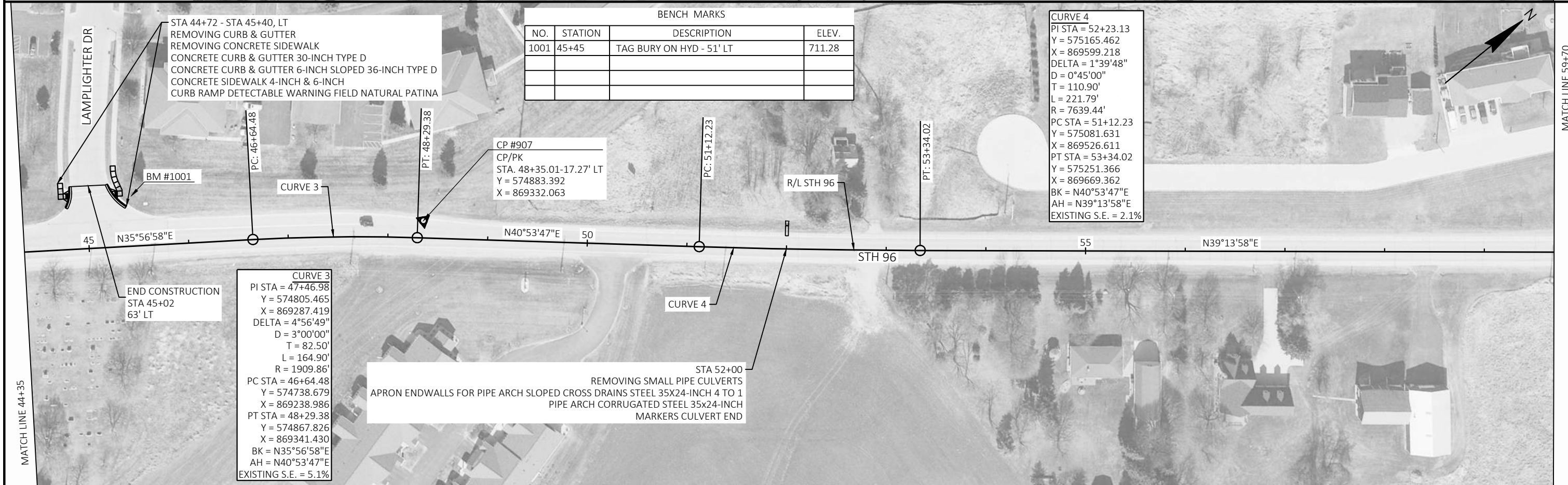
CURVE 2
 PI STA = 38+34.96
 Y = 574067.120
 X = 868751.975
 DELTA = 3°02'10"
 D = 1°38'13"
 T = 92.76'
 L = 185.47'
 R = 3500.00'
 PC STA = 37+42.21
 Y = 573989.252
 X = 868701.574
 PT STA = 39+27.68
 Y = 574142.209
 X = 868806.429
 BK = N32°54'48"E
 AH = N35°56'58"E
 EXISTING S.E. = 3.7%

BEGIN PROJECT 4075-35-71
 STA 33+50.00
 Y=573,659.995
 X=868,488.460

STA 39+78
 CLEANING CULVERT PIPES
 MARKERS CULVERT END

5

5



BENCH MARKS

NO.	STATION	DESCRIPTION	ELEV.
1001	45+45	TAG BURY ON HYD - 51' LT	711.28

STA 44+72 - STA 45+40, LT
 REMOVING CURB & GUTTER
 REMOVING CONCRETE SIDEWALK
 CONCRETE CURB & GUTTER 30-INCH TYPE D
 CONCRETE CURB & GUTTER 6-INCH SLOPED 36-INCH TYPE D
 CONCRETE SIDEWALK 4-INCH & 6-INCH
 CURB RAMP DETECTABLE WARNING FIELD NATURAL PATINA

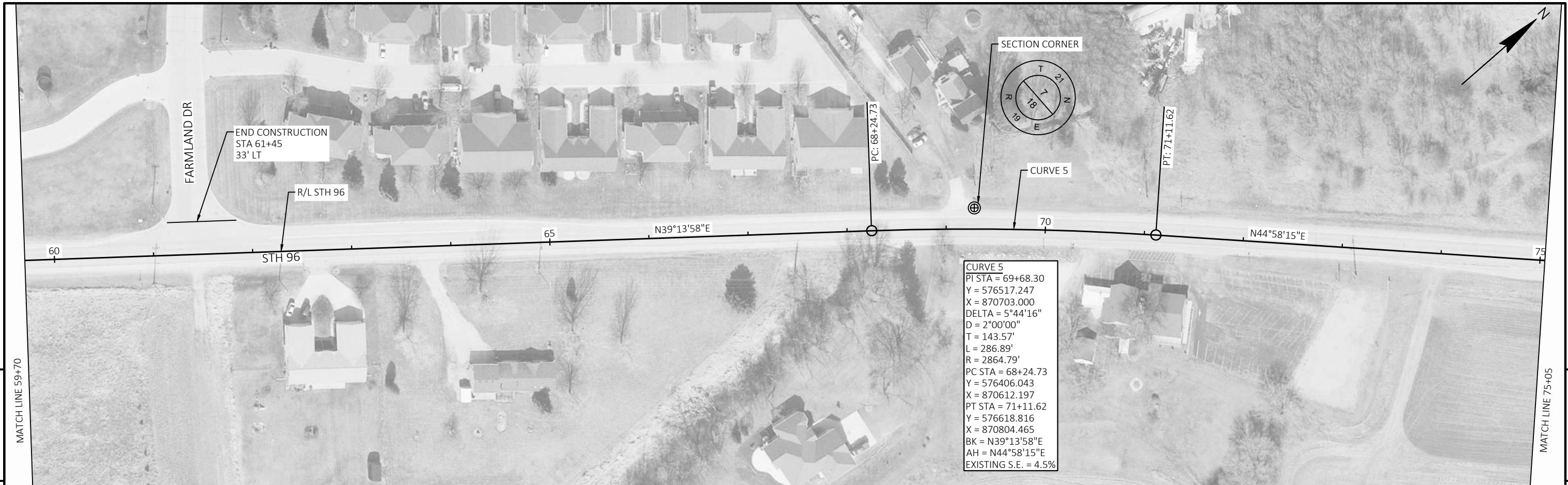
CURVE 4
 PI STA = 52+23.13
 Y = 575165.462
 X = 869599.218
 DELTA = 1°39'48"
 D = 0°45'00"
 T = 110.90'
 L = 221.79'
 R = 7639.44'
 PC STA = 51+12.23
 Y = 575081.631
 X = 869526.611
 PT STA = 53+34.02
 Y = 575251.366
 X = 869669.362
 BK = N40°53'47"E
 AH = N39°13'58"E
 EXISTING S.E. = 2.1%

CURVE 3
 PI STA = 47+46.98
 Y = 574805.465
 X = 869287.419
 DELTA = 4°56'49"
 D = 3°00'00"
 T = 82.50'
 L = 164.90'
 R = 1909.86'
 PC STA = 46+64.48
 Y = 574738.679
 X = 869238.986
 PT STA = 48+29.38
 Y = 574867.826
 X = 869341.430
 BK = N35°56'58"E
 AH = N40°53'47"E
 EXISTING S.E. = 5.1%

STA 52+00
 REMOVING SMALL PIPE CULVERTS
 APRON ENDWALLS FOR PIPE ARCH SLOPED CROSS DRAINS STEEL 35X24-INCH 4 TO 1
 PIPE ARCH CORRUGATED STEEL 35X24-INCH
 MARKERS CULVERT END

MATCH LINE 44+35

MATCH LINE 59+70

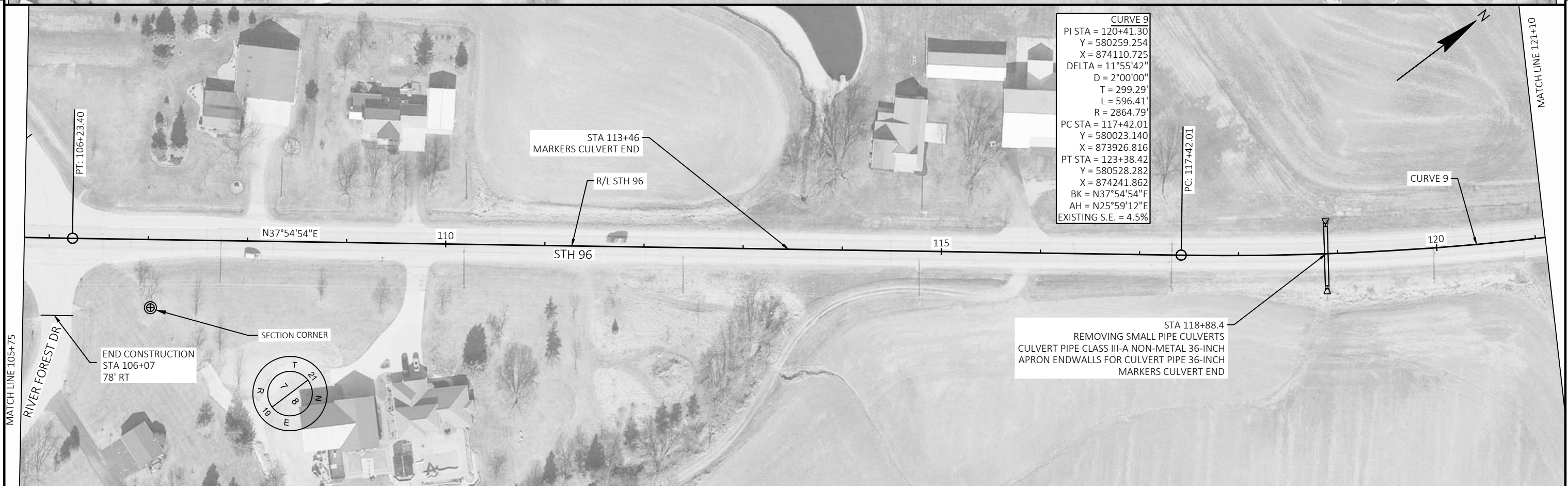


PROJECT NO: 4075-35-71	HWY: STH 96	COUNTY: OUTAGAMIE	PLAN SHEETS	SHEET	E
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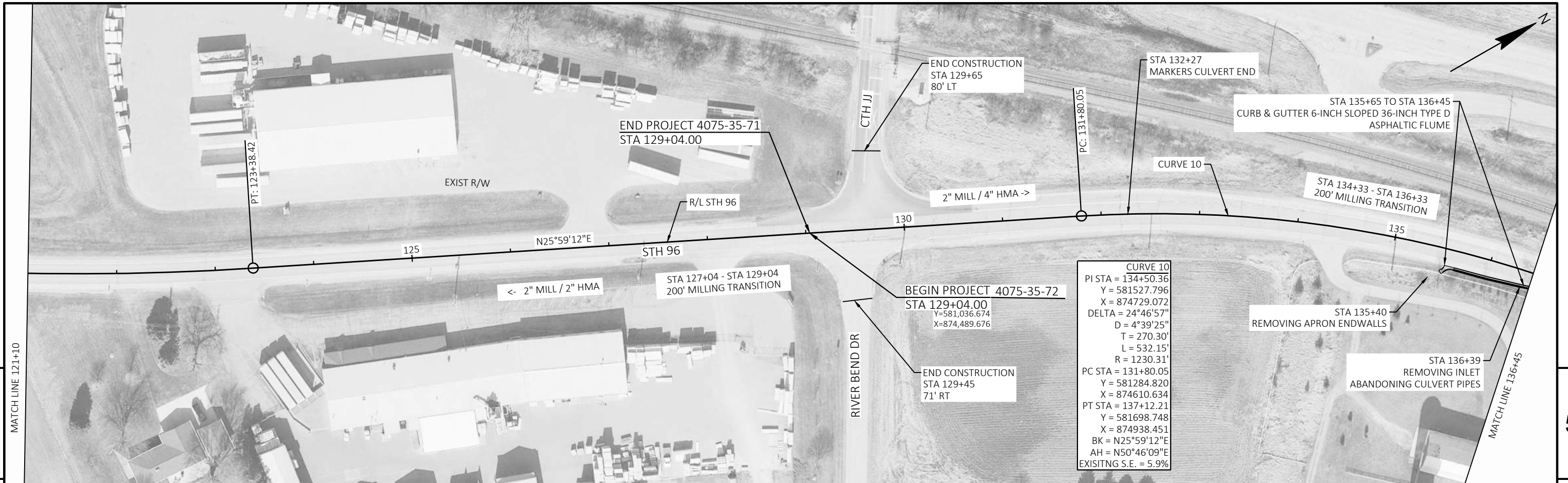


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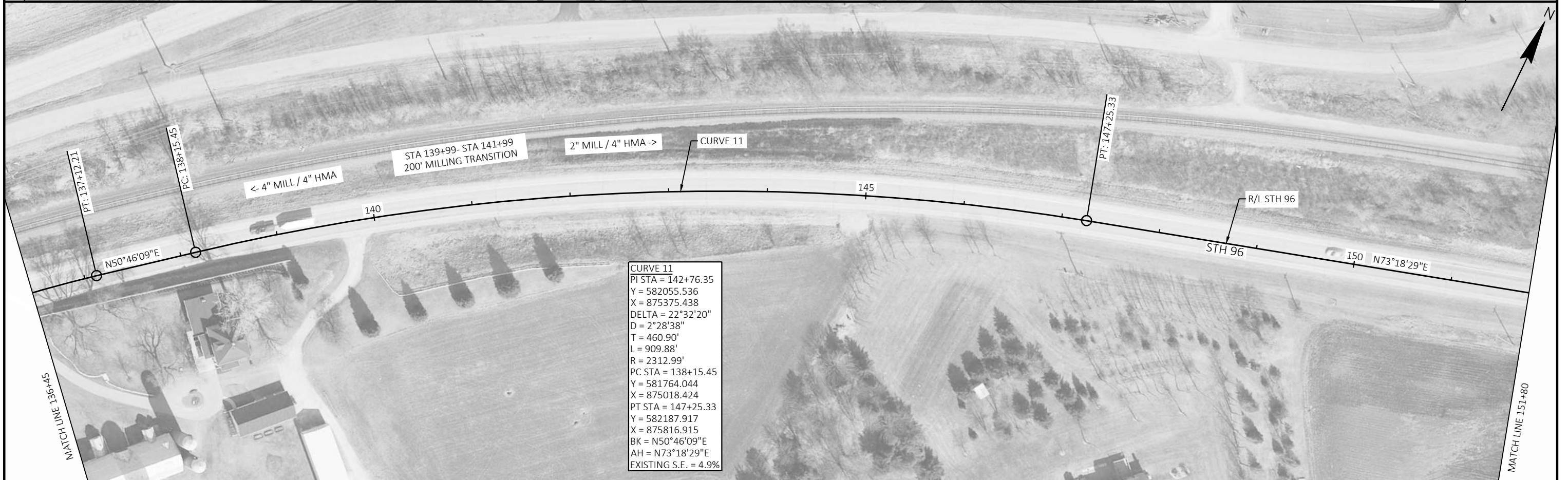
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PROJECT NO: 4075-35-71	HWY: STH 96	COUNTY: OUTAGAMIE	PLAN SHEETS	SHEET	E
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CURVE 10
 PI STA = 134+50.36
 Y = 581527.796
 X = 874729.072
 DELTA = 24°46'57"
 D = 4°39'25"
 T = 270.30'
 L = 532.15'
 R = 1230.31'
 PC STA = 131+80.05
 Y = 581284.820
 X = 874610.634
 PT STA = 137+12.21
 Y = 581698.748
 X = 874938.451
 BK = N25°59'12"E
 AH = N50°46'09"E
 EXISTING S.E. = 5.9%



CURVE 11
 PI STA = 142+76.35
 Y = 582055.536
 X = 875375.438
 DELTA = 22°32'20"
 D = 2°28'38"
 T = 460.90'
 L = 909.88'
 R = 2312.99'
 PC STA = 138+15.45
 Y = 581764.044
 X = 875018.424
 PT STA = 147+25.33
 Y = 582187.917
 X = 875816.915
 BK = N50°46'09"E
 AH = N73°18'29"E
 EXISTING S.E. = 4.9%

PROJECT NO: 4075-35-71/72	HWY: STH 96	COUNTY: OUTAGAMIE	PLAN SHEETS	SHEET	E
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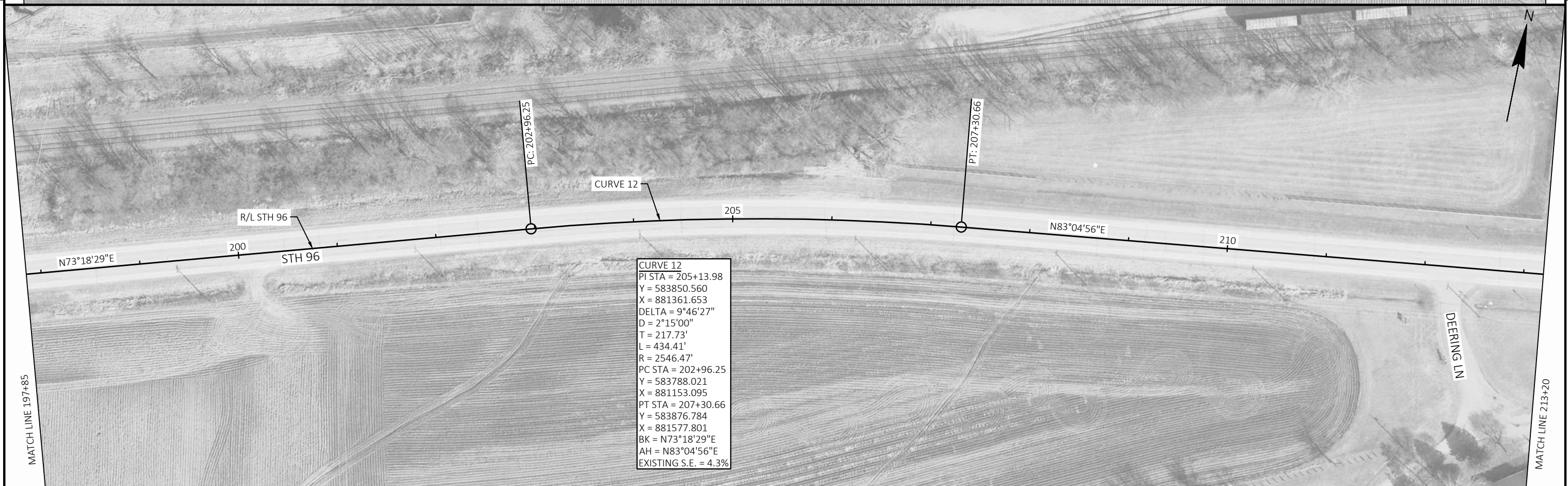
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PROJECT NO: 4075-35-72	HWY: STH 96	COUNTY: OUTAGAMIE	PLAN SHEETS	SHEET	E
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5
MATCH LINE 182+50

5
MATCH LINE 197+85



MATCH LINE 197+85

MATCH LINE 213+20

CURVE 12
 PI STA = 205+13.98
 Y = 583850.560
 X = 881361.653
 DELTA = 9°46'27"
 D = 2°15'00"
 T = 217.73'
 L = 434.41'
 R = 2546.47'
 PC STA = 202+96.25
 Y = 583788.021
 X = 881153.095
 PT STA = 207+30.66
 Y = 583876.784
 X = 881577.801
 BK = N73°18'29"E
 AH = N83°04'56"E
 EXISTING S.E. = 4.3%

PROJECT NO: 4075-35-72 HWY: STH 96 COUNTY: OUTAGAMIE PLAN SHEETS SHEET **E**



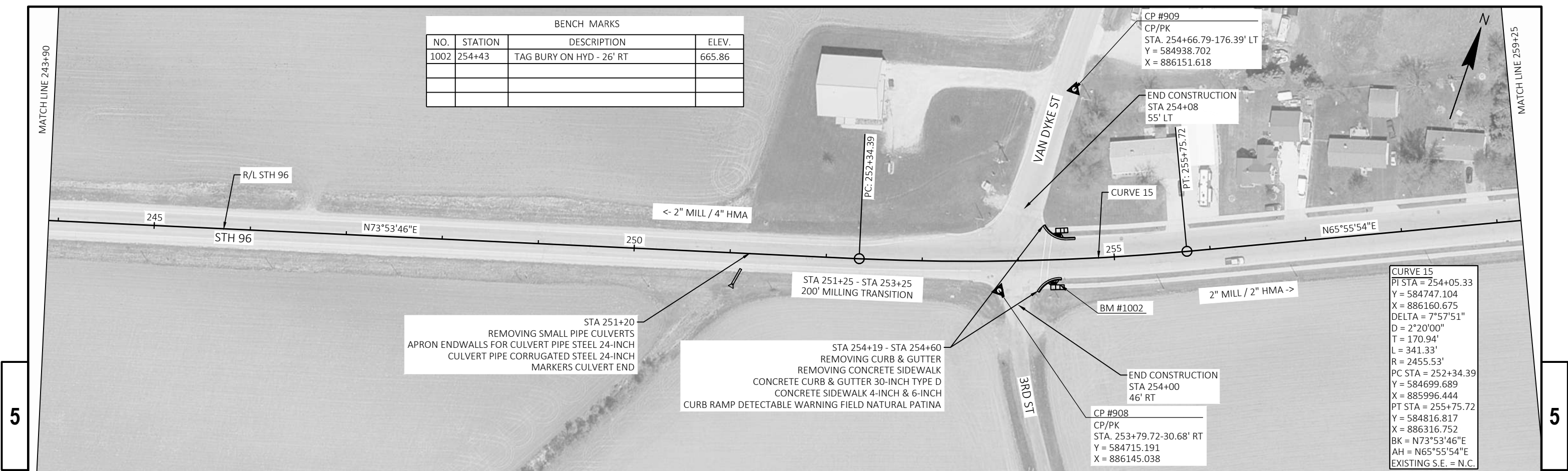
CURVE 13
 PI STA = 227+44.16
 Y = 584119.296
 X = 883576.648
 DELTA = 4°16'20"
 D = 1°30'00"
 T = 142.48'
 L = 284.82'
 R = 3819.71'
 PC STA = 226+01.68
 Y = 584102.135
 X = 883435.208
 PT STA = 228+86.51
 Y = 584146.945
 X = 883716.415
 BK = N83°04'56"E
 AH = N78°48'36"E
 EXISTING S.E. = N.C.



CURVE 14
 PI STA = 240+70.06
 Y = 584376.628
 X = 884877.468
 DELTA = 4°54'51"
 D = 0°52'00"
 T = 283.68'
 L = 567.00'
 R = 6611.04'
 PC STA = 237+86.38
 Y = 584321.578
 X = 884599.185
 PT STA = 243+53.39
 Y = 584455.315
 X = 885150.012
 BK = N78°48'36"E
 AH = N73°53'46"E
 EXISTING S.E. = 2.2%

PROJECT NO: 4075-35-72	HWY: STH 96	COUNTY: BROWN & OUTAGAMIE	PLAN SHEETS	SHEET	E
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BENCH MARKS			
NO.	STATION	DESCRIPTION	ELEV.
1002	254+43	TAG BURY ON HYD - 26' RT	665.86

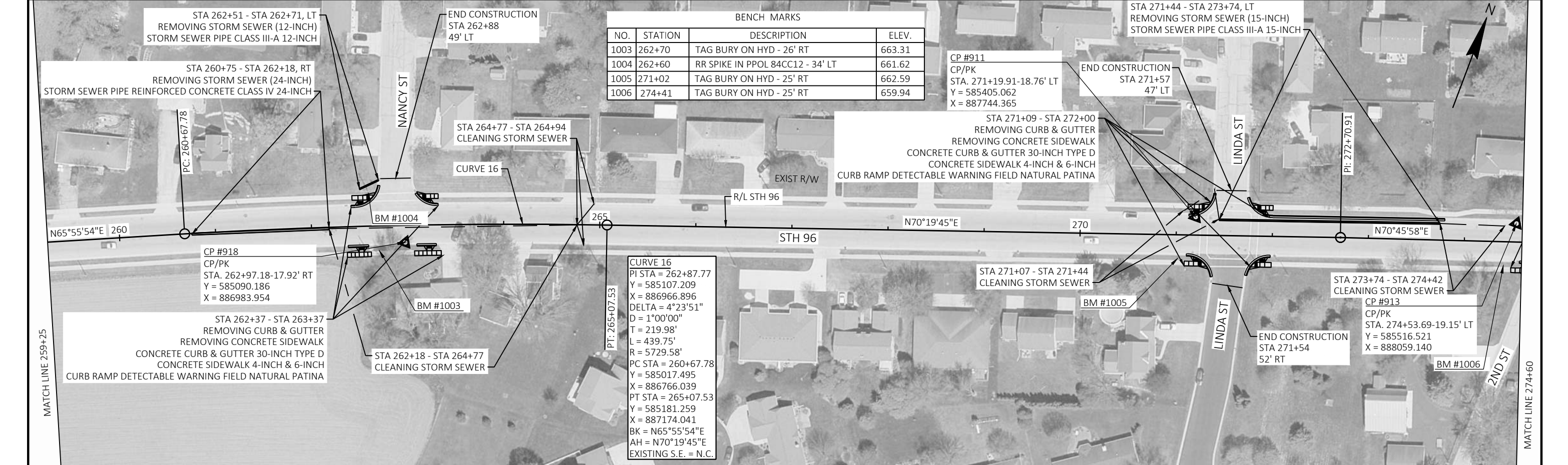


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CURVE 15	
PI STA = 254+05.33	Y = 584747.104
X = 886160.675	DELTA = 7°57'51"
D = 2°20'00"	T = 170.94'
L = 341.33'	R = 2455.53'
PC STA = 252+34.39	Y = 584699.689
X = 885996.444	PT STA = 255+75.72
Y = 584816.817	X = 886316.752
BK = N73°53'46"E	AH = N65°55'54"E
EXISTING S.E. = N.C.	

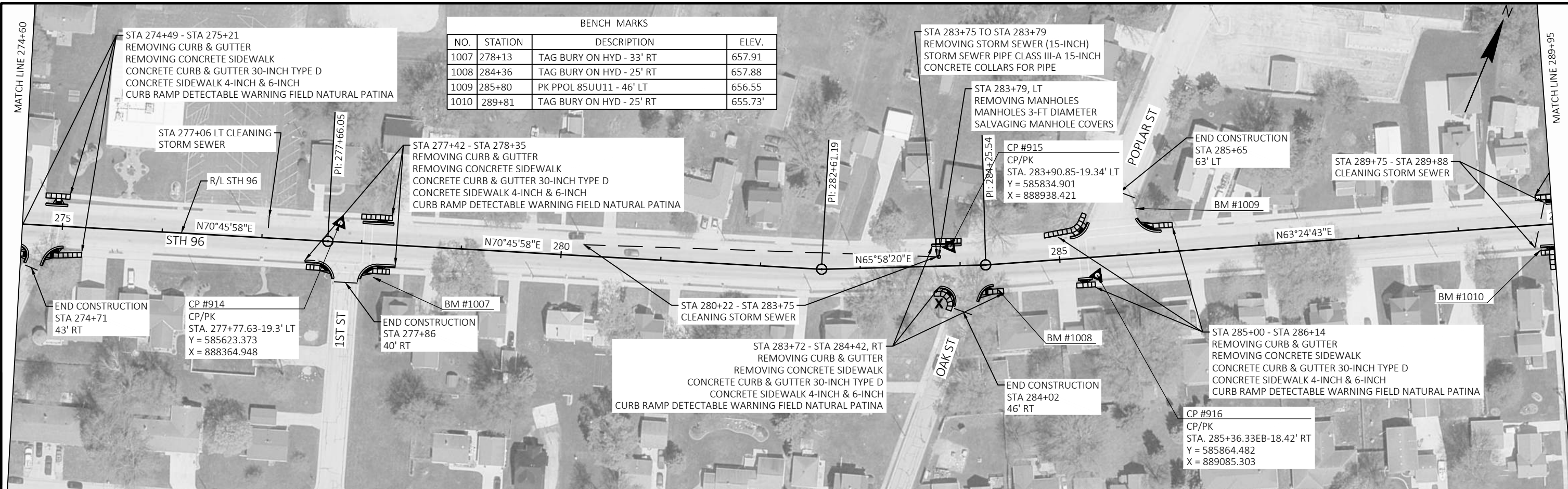
BENCH MARKS			
NO.	STATION	DESCRIPTION	ELEV.
1003	262+70	TAG BURY ON HYD - 26' RT	663.31
1004	262+60	RR SPIKE IN PPOL 84CC12 - 34' LT	661.62
1005	271+02	TAG BURY ON HYD - 25' RT	662.59
1006	274+41	TAG BURY ON HYD - 25' RT	659.94



5

5

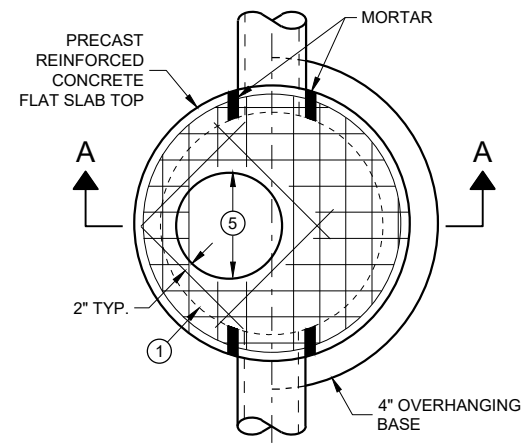
CURVE 16	
PI STA = 262+87.77	Y = 585107.209
X = 886966.896	DELTA = 4°23'51"
D = 1°00'00"	T = 219.98'
L = 439.75'	R = 5729.58'
PC STA = 260+67.78	Y = 585017.495
X = 886766.039	PT STA = 265+07.53
Y = 585181.259	X = 887174.041
BK = N65°55'54"E	AH = N70°19'45"E
EXISTING S.E. = N.C.	



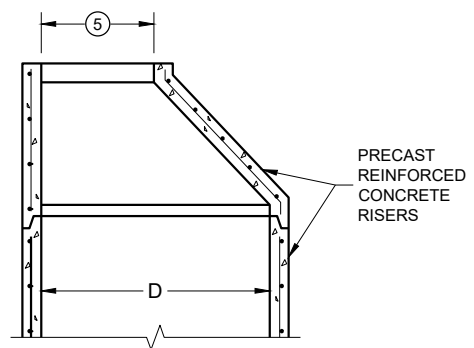
PROJECT NO: 4075-35-72	HWY: STH 96	COUNTY: BROWN	PLAN SHEETS	SHEET	E
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Standard Detail Drawing List

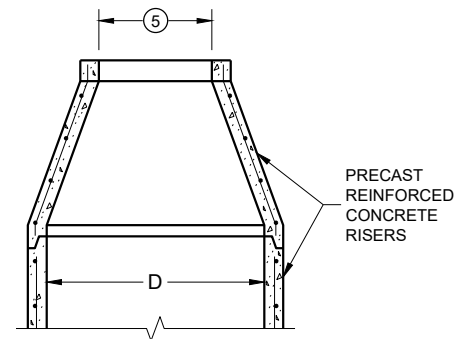
08B09-03	MANHOLES 3-FT, 4-FT, 5-FT, 6-FT, 7-FT, 8-FT, 9-FT, 10-FT DIAMETER
08D01-23A	CONCRETE CURB & GUTTER
08D01-23B	CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS
08D04-07	CONCRETE SURFACE DRAINS & ASPHALTIC FLUMES
08D05-20A	CURB RAMPS TYPES 1 AND 1-A
08D05-20B	CURB RAMPS TYPES 2 AND 3
08D05-20C	CURB RAMPS TYPES 4A AND 4A1
08D05-20D	CURB RAMPS TYPE 4B AND 4B1
08D05-20E	CURB RAMPS TYPES 5, 6, 7A, 7B & 8
08D05-20F	CURB RAMPS RADIAL DETECTABLE WARNING FIELD APPLICATIONS
08D05-20G	CURB RAMPS RECTANGULAR AND RADIAL DETECTABLE WARNING PLATES
08D21-01	DRIVEWAYS WITHOUT CURB & GUTTER
08D22-01	DRIVEWAYS WITHOUT CURB & GUTTER RESURFACING PROJECTS RURAL
08E09-06	SILT FENCE
08E10-02	INLET PROTECTION TYPE A, B, C AND D
08E15-01	CULVERT PIPE CHECK
08F01-11	APRON ENDWALLS FOR CULVERT PIPE
08F02-01	APRON ENDWALLS FOR PIPE ARCH AND ELLIPTICAL PIPE
08F04-08	JOINT TIES FOR CONCRETE PIPE AND CONCRETE COLLAR DETAIL
13A10-03A	SHOULDER RUMBLE STRIPS - ASPHALT
13A10-03G	SHOULDER AND EDGE LINE RUMBLE STRIPS - CROSSINGS, INTERSECTIONS, BRIDGES, DRIVEWAYS
13A10-03H	SHOULDER AND EDGE LINE RUMBLE STRIPS - RAILROAD, PASSING, CLIMBING AND BYPASS LANES
13A11-04A	CENTERLINE RUMBLE STRIPS - ASPHALT
13A11-04D	CENTERLINE RUMBLE STRIPS - INTERSECTIONS, DRIVEWAYS, BRIDGES, RAILROADS
13C19-03	HMA LONGITUDINAL JOINTS
15A03-02A	FLEXIBLE MARKER POST FOR CULVERT END
15A03-02B	FLEXIBLE MARKER POST FOR CULVERT END
15C02-09A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-09B	BARRICADES AND SIGNS FOR VARIOUS CLOSURES
15C02-09C	DETOUR SIGNING FOR MAINLINE CLOSURES
15C04-05	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M.P.H. OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC
15C05-05	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M.P.H. OR LESS
15C08-23A	PERMANENT LONGITUDINAL PAVEMENT MARKINGS
15C08-23B	TEMPORARY LONGITUDINAL PAVEMENT MARKING
15C11-10B	CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS
15C12-09A	TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION
15C12-09B	TRAFFIC CONTROL, LANE CLOSURE WITH AUTOMATED FLAGGER ASSISTANCE DEVICE
15C19-08A	MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY
15C33-04	STOP LINE AND CROSSWALK PAVEMENT MARKING
15C35-06A	PAVEMENT MARKING (INTERSECTIONS)
15D28-04	TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY
15D29-06	TRAFFIC CONTROL, VEHICLE ENTRANCE/EXIT OR HAUL ROAD
15D30-09A	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09B	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09C	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09F	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09G	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09H	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09I	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09K	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09L	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D39-02	TRAFFIC CONTROL, DROP-OFF SIGNING
15D44-02	TRAFFIC CONTROL, SIGNING ON ROADWAYS WITH MILLED SURFACES
15D45-03	TRAFFIC CONTROL, SIGNING ON ROADWAYS WITH LOOSE GRAVEL
15D51-01	TRAFFIC CONTROL, MOBILE OPERATIONS ON AN UNDIVIDED ROADWAY



**PLAN VIEW
CIRCULAR OPENING**



**OPTIONAL PRECAST
REINFORCED CONCRETE
ECCENTRIC TOP**



**OPTIONAL PRECAST
REINFORCED CONCRETE
CONCENTRIC TOP**

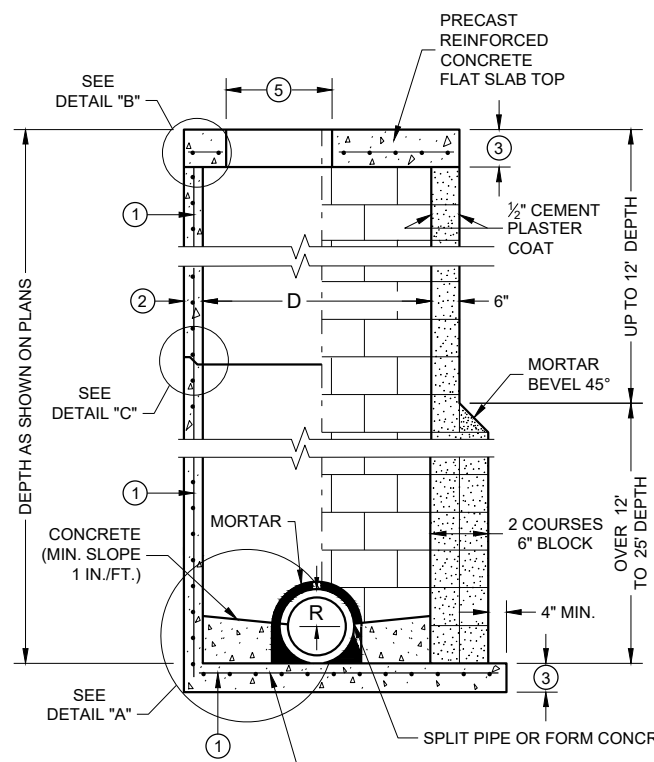
MANHOLE COVER OPENING MATRIX

MANHOLE COVER TYPE OPENING SIZE (FT.)	C	ALL J'S	K	L	M
2 DIA.	X	X		X	
3 DIA.			X		X

PIPE MATRIX

MANHOLE SIZE (DIA.)	MAXIMUM INSIDE PIPE DIAMETER FOR TWO PIPES		MINIMUM WALL THICKNESS (IN)	MINIMUM PRECAST FLAT SLAB TOP AND BASE THICKNESS
	180° SEPARATION (IN)	90° SEPARATION (IN)		
3-FT	15	12	4	6
4-FT	24	18	4	6
5-FT	36	24	5	8
6-FT	42	36	6	8
7-FT	48	36/42*	7	8
8-FT	60	42	8	8
9-FT	66	54	9	10
10-FT	72	60	10	10

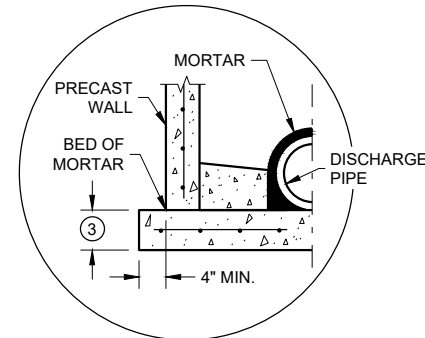
*A 36" PIPE AND A 42" PIPE CAN BE PLACED WITHIN 90 DEGREES. SEE MINIMUM HORIZONTAL PIPE SEPARATION DETAIL.



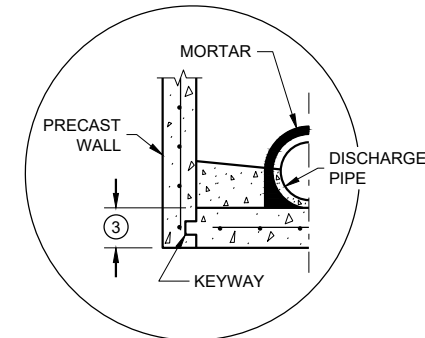
SECTION A - A

PRECAST REINFORCED CONCRETE WITH MONOLITHIC BASE

CONCRETE BLOCK WITH CAST IN PLACE OR PRECAST REINFORCED CONCRETE BASE ①

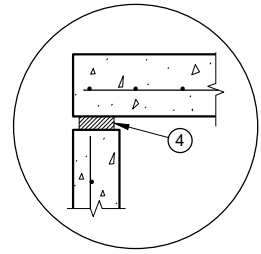


SEPARATE PRECAST REINFORCED CONCRETE BASE OPTION

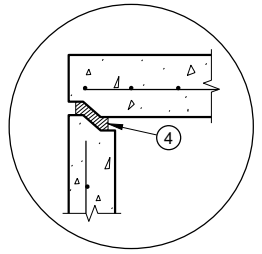


PRECAST REINFORCED CONCRETE WITH INTEGRAL BASE OPTION

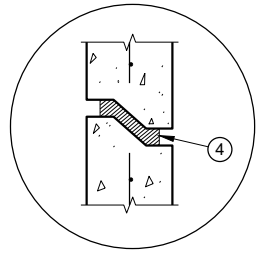
DETAIL "A"



TOP WITH PLAIN END JOINT



TOP WITH TONGUE AND GROOVE JOINT



RISER WITH TONGUE AND GROOVE JOINT

DETAIL "B"

DETAIL "C"

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST MANHOLE UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL DRAINAGE STRUCTURES ARE DESIGNATED ON THE PLANS AS "MANHOLES 3X3-L", "CATCH BASINS 4-B", "INLETS 2X3-H", ETC. THE FIRST NUMBERS DESIGNATE THE SIZE OF THE STRUCTURE, AND THE FOLLOWING LETTER DESIGNATES THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF FOUNDATION BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

PRECAST REINFORCED CONCRETE CONE TOPS (ECCENTRIC OR CONCENTRIC) OR PRECAST REINFORCED CONCRETE FLAT SLAB TOPS MAY BE USED ON CONCRETE BLOCK STRUCTURES.

ECCENTRIC CONE TOPS MAY BE USED ON ALL STRUCTURES. CONCENTRIC CONE TOPS SHALL BE USED ONLY ON STRUCTURES 5 FEET OR LESS IN DEPTH UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

STEPS MEETING AASHTO M199 AND THE FOLLOWING REQUIREMENTS SHALL BE INSTALLED IN ALL STRUCTURES OVER 5 FEET IN DEPTH: 16 INCH C-C MAXIMUM SPACING; PROJECT A MINIMUM CLEAR DISTANCE OF 4 INCHES FROM THE WALL AT THE POINT OF EMBEDMENT; MINIMUM LENGTH OF 10 INCHES; MINIMUM WALL EMBEDMENT OF 3 INCHES. FERROUS METAL STEPS NOT PAINTED OR TREATED TO RESIST CORROSION SHALL HAVE A MINIMUM CROSS SECTIONAL DIMENSION OF 1 INCH.

STEPS OF APPROVED POLYPROPYLENE PLASTIC COATED REINFORCEMENT BAR ARE ACCEPTABLE. REINFORCING BAR MUST BE A MINIMUM OF 1/2 INCH AND MEET THE REQUIREMENTS OF ASTM A615.

CERTIFICATION SHALL BE PROVIDED THAT INSTALLED STEPS WHEN TESTED IN ACCORDANCE WITH SECTION 10 OF AASHTO T280 CAN WITHSTAND A VERTICAL LOAD OF 800 LBS. AND A HORIZONTAL LOAD OF 400 LBS.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

ALL PRECAST MANHOLE UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF AASHTO DESIGNATION M199.

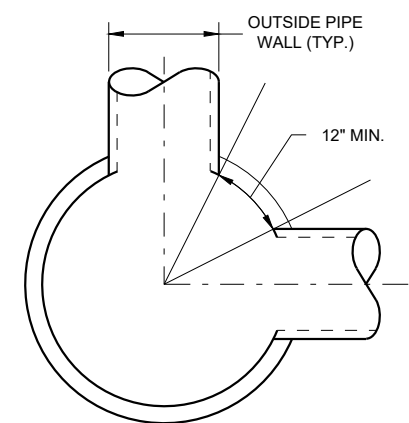
PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

CONCRETE BLOCK WILL NOT BE PERMITTED FOR STRUCTURES GREATER THAN 4 FEET IN DIAMETER.

4" OVERHANGING BASES ARE REQUIRED FOR ALL CONCRETE BLOCK INSTALLATIONS. 4" OVERHANG IS REQUIRED WHEN SEPARATE PRECAST BASE IS PROVIDED. OVERHANG IS NOT REQUIRED ON PRECAST STRUCTURES WITH AN INTEGRAL OR MONOLITHIC BASE.

FOR ADDITIONAL CONFIGURATIONS, MAINTAIN A MINIMUM OF 12 INCHES AS MEASURED FROM THE INSIDE OF THE STRUCTURE WALL BETWEEN THE OUTSIDE PIPE WALLS OF ADJACENT PIPES. SEE DETAIL "D".

- ① FOR PRECAST MANHOLES PROVIDE REINFORCING STEEL IN ACCORDANCE TO AASHTO M199.
- ② SEE PIPE MATRIX TABLE FOR MINIMUM WALL THICKNESS FOR PRECAST MANHOLES
- ③ SEE PIPE MATRIX TABLE FOR MINIMUM THICKNESS OF PRECAST FLAT SLAB TOPS AND BASES.
- ④ JOINTS TO BE SEALED WITH A BUTYL RUBBER SEAL PER SEALANT MANUFACTURERS RECOMMENDATIONS CONFORMING TO ASTM C 990 (TYP.).
- ⑤ SEE MANHOLE COVER OPENING MATRIX.

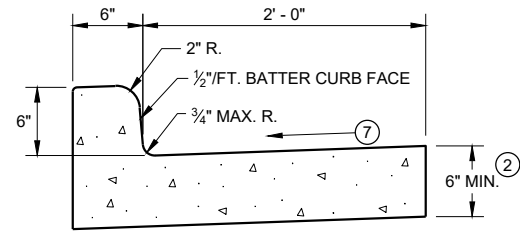


MINIMUM HORIZONTAL PIPE SEPARATION

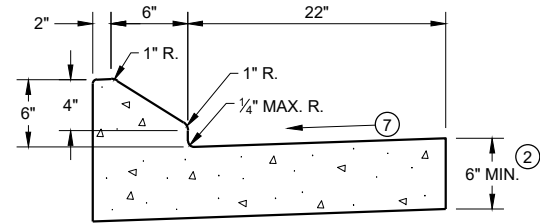
**MANHOLES, 3-FT, 4-FT
5-FT, 6-FT, 7-FT, 8-FT, 9-FT
AND 10-FT DIAMETER**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

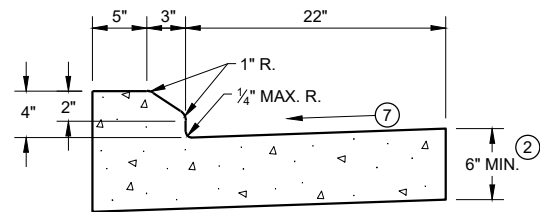
APPROVED
November 2021 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA



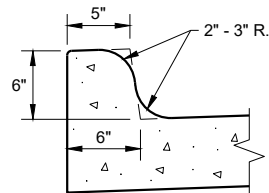
TYPES A^① & D



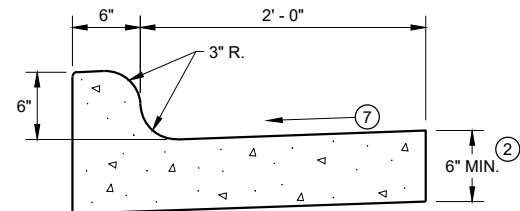
6" SLOPED CURB TYPES G^① & J



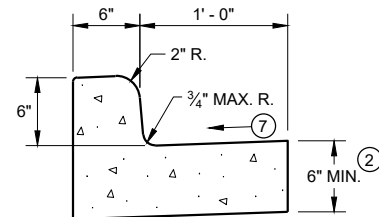
4" SLOPED CURB TYPES G^① & J



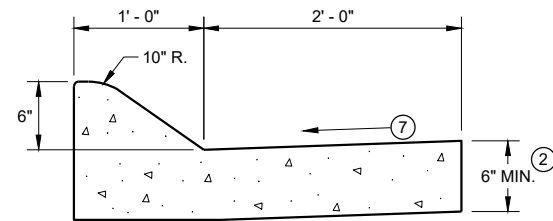
TYPES K^① & L
(OPTIONAL CURB SHAPE)



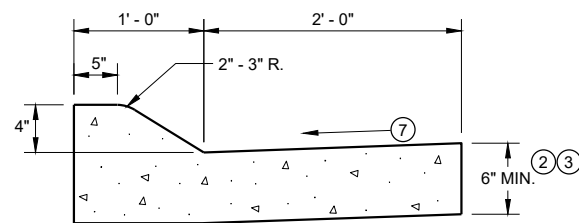
TYPES K^① & L
CONCRETE CURB AND GUTTER 30"



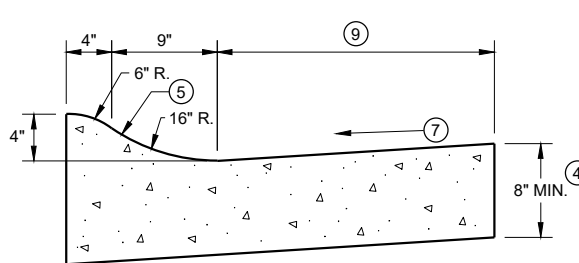
TYPES A^① & D
CONCRETE CURB AND GUTTER 18"



6" SLOPED CURB TYPES A^① & D

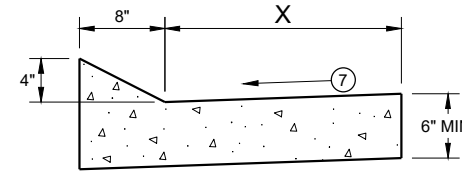


4" SLOPED CURB TYPES A^① & D
CONCRETE CURB AND GUTTER 36"



4" SLOPED CURB TYPES R^① & T

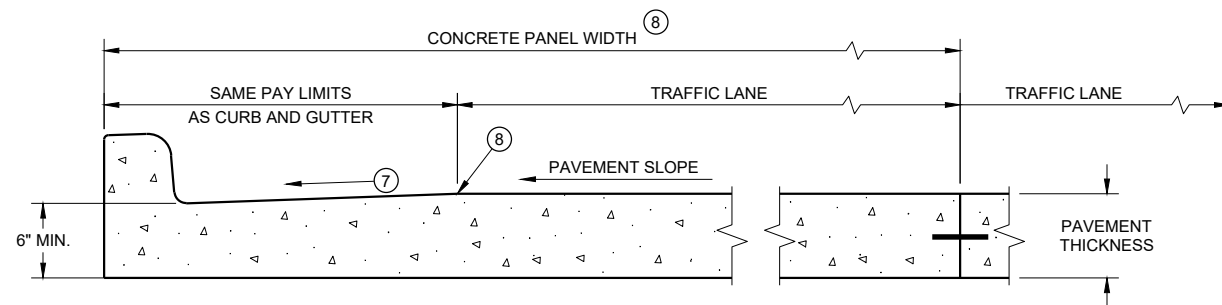
TBT & TBTT	X
30"	22"
36"	28"



TYPES TBT & TBTT^①
CONCRETE CURB AND GUTTER

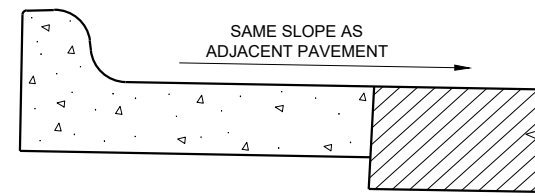
PAVEMENT THICKNESS AND MAXIMUM CONCRETE PANEL WIDTH TABLE

PAVEMENT THICKNESS	MAXIMUM PANEL WIDTH
LESS THAN 10"	12'
10" & ABOVE	15'



PARTIAL SECTION OF PAVEMENT* WITH INTEGRAL CURB AND GUTTER

* BIKE LANE IS NOT SHOWN



REVERSE SLOPE GUTTER^⑥
(TYPICAL FOR ALL CURB & GUTTER TYPES)

GENERAL NOTES

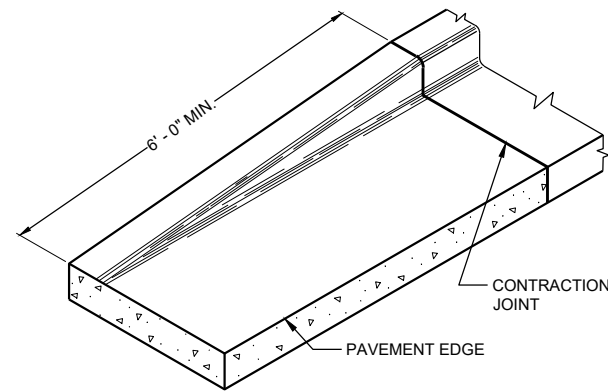
DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

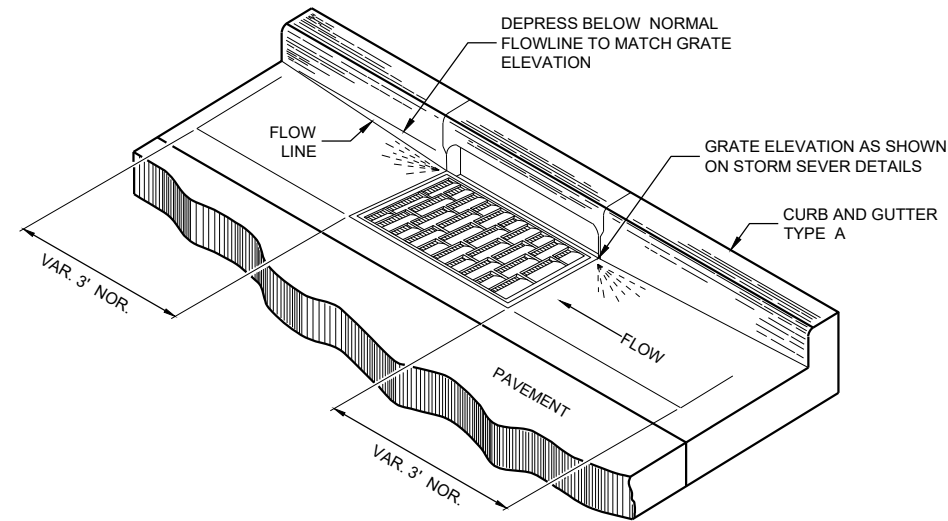
INTEGRAL CURB AND GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB AND GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ③ USE 8" MINIMUM GUTTER THICKNESS WHEN USED WITH AN ADJACENT CONCRETE TRUCK APRON PLACED BEHIND BACK OF CURB.
- ④ THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑤ UNLESS OTHERWISE NOTED, FOR STAKING PURPOSES THE FACE OF CURB IS 6" FROM THE BACK OF CURB.
- ⑥ WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.
- ⑦ USE 4% GUTTER CROSS SLOPE UNLESS OTHERWISE NOTED IN THE PLANS.
- ⑧ INCLUDE LONGITUDINAL JOINT AND TIE BARS ALONG LANE EDGE WHEN CONCRETE PANEL WIDTH EXCEEDS THE MAXIMUM WIDTH PER TABLE BELOW. LONGITUDINAL JOINT(S) ARE NOT ALLOWED WITHIN TRAFFIC LANES AND BIKE LANES. LONGITUDINAL JOINT MAY BE SAWED.
- ⑨ CONCRETE CURB AND GUTTER 4-INCH SLOPED 30-INCH TYPE "R" AND "T" = 17 INCHES
CONCRETE CURB AND GUTTER 4-INCH SLOPED 36-INCH TYPE "R" AND "T" = 23 INCHES



END SECTION CURB AND GUTTER



DETAIL OF CURB AND GUTTER AT INLETS

(TYPICAL H INLET COVER SHOWN)

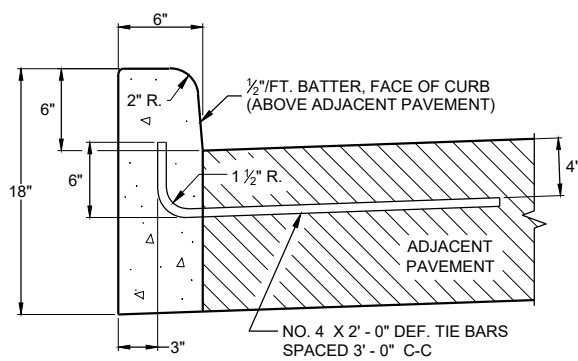
GENERAL NOTES

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

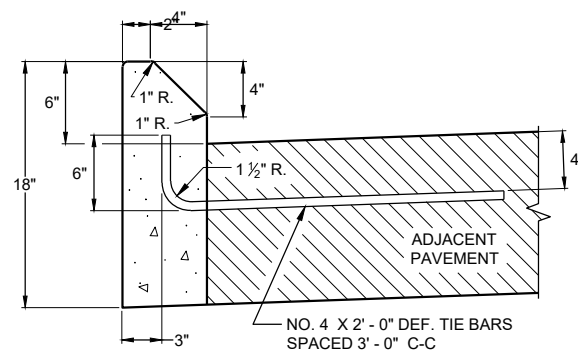
PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

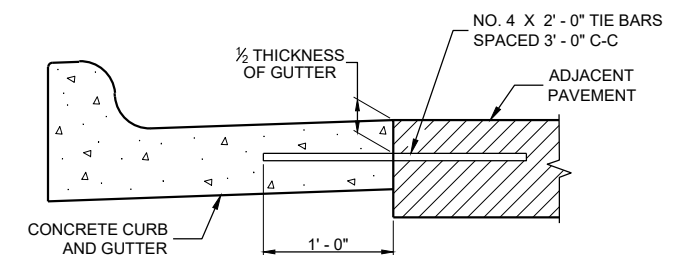
- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑩ REFER TO SDD 08D18 AND 08D19 FOR ADDITIONAL DRIVEWAY ENTRANCE CURB DETAILS.
- ⑪ PLACE 1" THICK EXPANSION JOINT MATERIAL BETWEEN VERTICAL FACE CURB TYPES EXTENDING FROM THE TOP OF CURB TO 1 INCH BELOW THE ADJOINING CONCRETE SURFACE. RIGID CONCRETE STRUCTURES INCLUDE RAISED CONCRETE MEDIANS, CONCRETE SAFETY ISLANDS, SPLITTER ISLANDS, OR LOCATIONS IDENTIFIED ON THE PLANS.



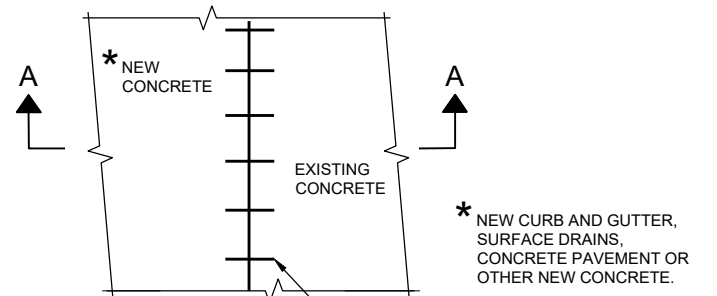
TYPES A^① & D



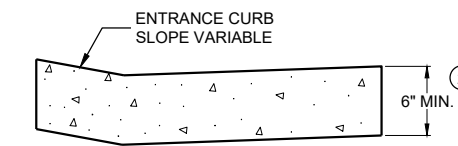
**TYPES G^① & J
CONCRETE CURB**



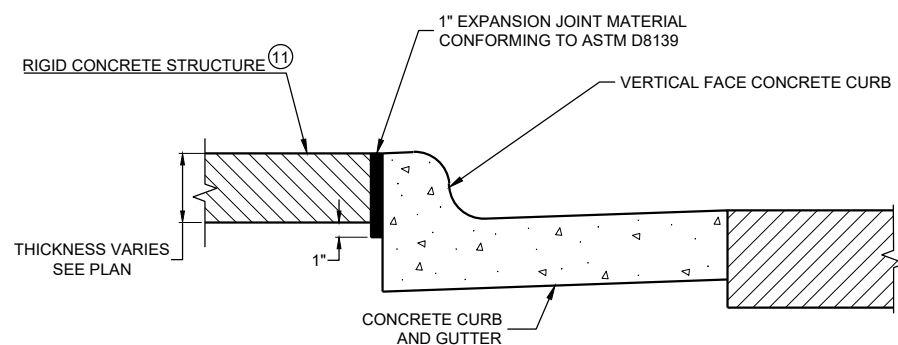
TYPICAL TIE BAR LOCATION^①



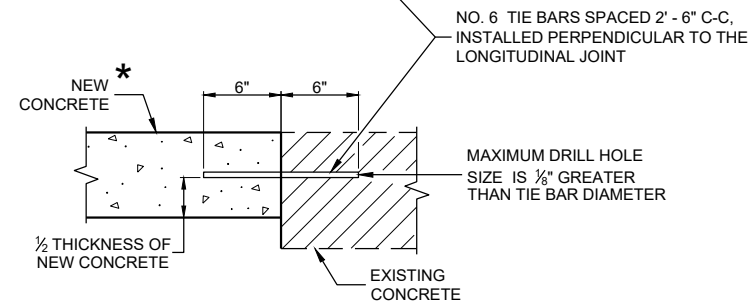
PLAN VIEW



**DRIVEWAY ENTRANCE CURB^⑩
(WHEN DIRECTED BY THE ENGINEER)**



EXPANSION JOINT DETAIL FOR VERTICAL CURB ABUTTING A RIGID STRUCTURE^⑪



**SECTION A - A
TIE BARS DRILLED INTO EXISTING PAVEMENT**

CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS

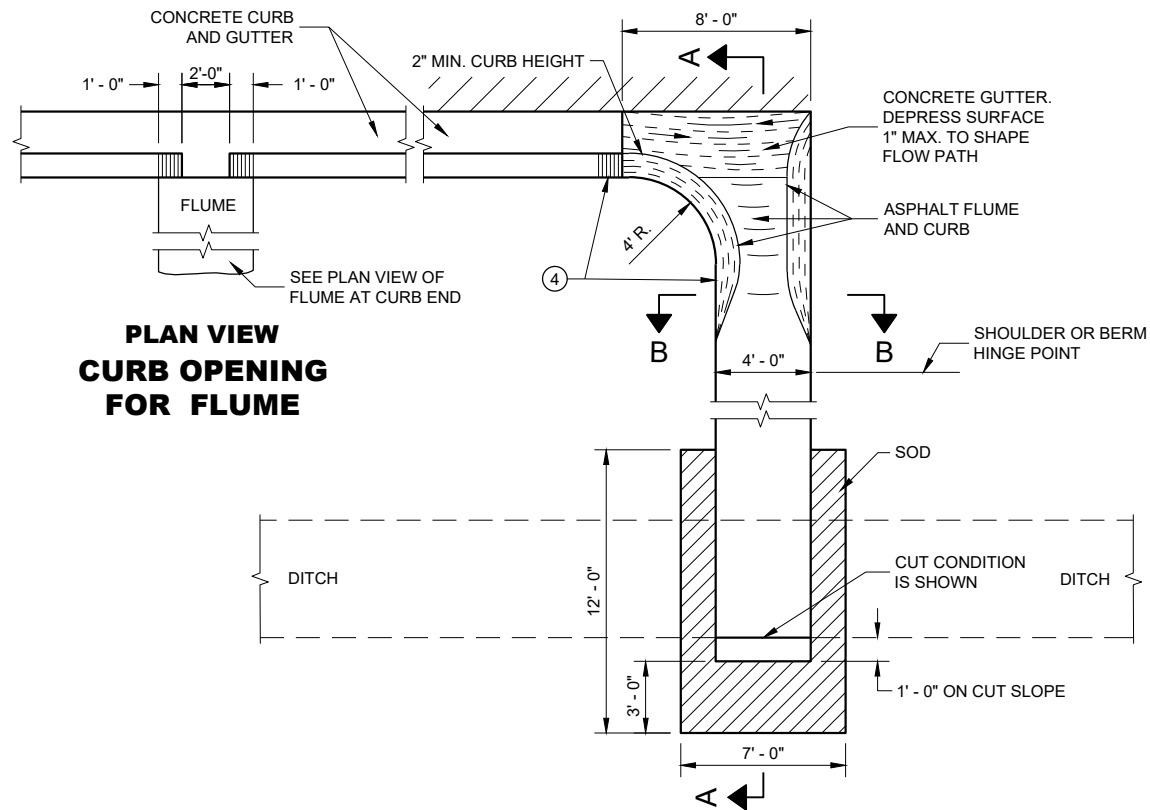
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE May 2023 /S/ Rodney Taylor
ROADWAY STANDARDS DEVELOPMENT ENGINEER

FHWA

NOTE: TAPER CURB ENDS TO GUTTER IN 1' - 0"

ASPHALTIC FLUME



**PLAN VIEW
CURB OPENING
FOR FLUME**

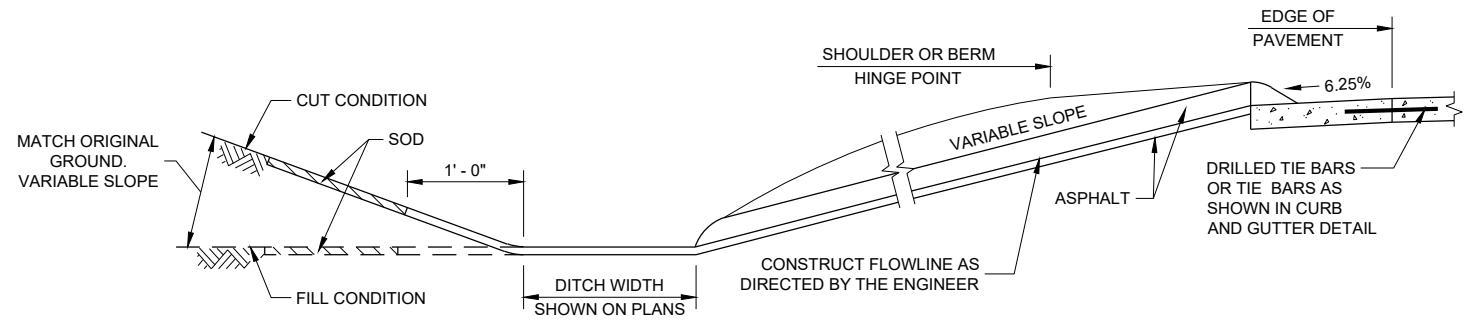
**PLAN VIEW
FLUME AT CURB END**

GENERAL NOTES

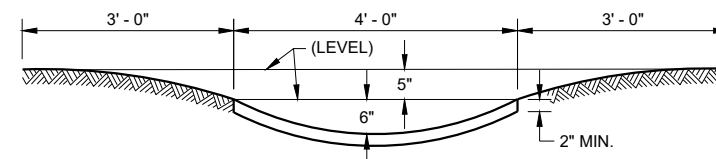
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

4" X 4" - W3.0 X W3.0 CONCRETE REINFORCEMENT SHALL BE IN ACCORDANCE WITH AASHTO SPECIFICATION M55.

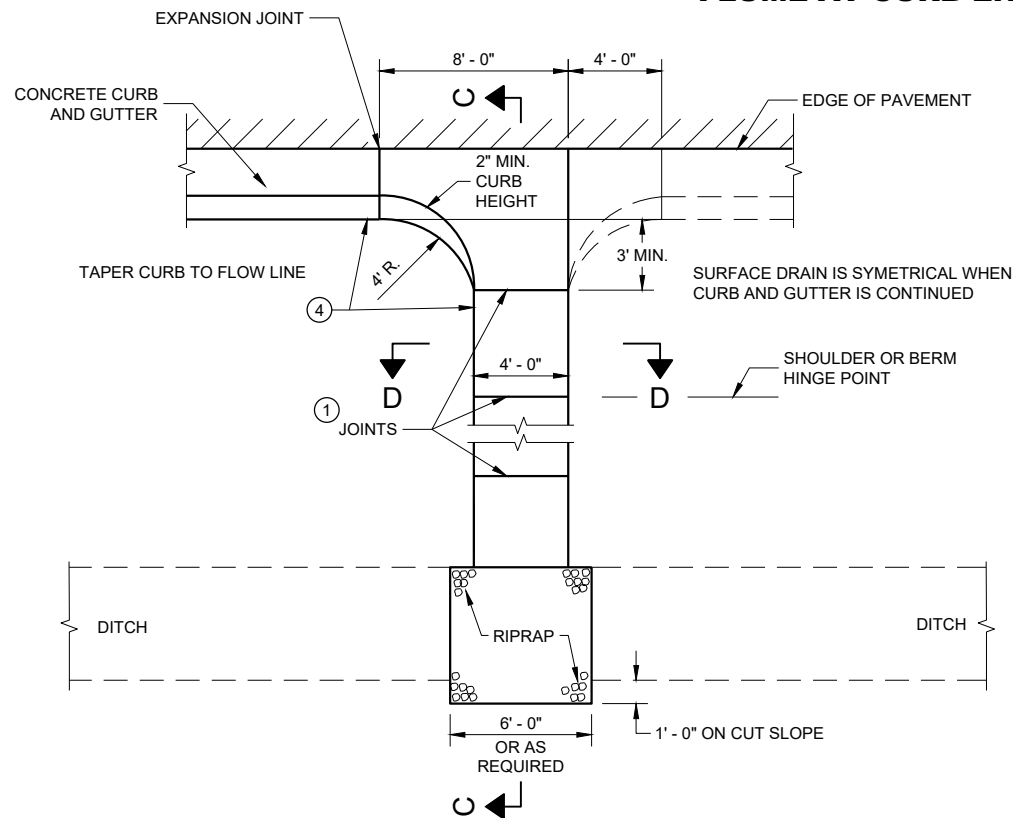
- ① JOINTS SHALL BE 1/8" TO 1/4" WIDE BY 1 1/2" DEEP AND SPACED AT UNIFORM INTERVALS OF APPROXIMATELY 4 FEET.
- ② GEOTEXTILE TYPE "R" SHALL UNDERLAY THE FULL LENGTH AND WIDTH OF THE CONCRETE SURFACE DRAIN AND RIPRAP.
- ③ CONCRETE SURFACE DRAIN WITHOUT CURB AND GUTTER MAY BE USED ON BACKSLOPES WHEN SPECIFIED.
- ④ ANGLE OF FLUME IN RELATION TO BACK OF CURB TO BE CONSTRUCTED PER THE PLAN DETAILS OR AS DIRECTED BY THE ENGINEER. ANGLE OF FLUME MAY BE OTHER THAN 90 DEGREES AS SHOWN.



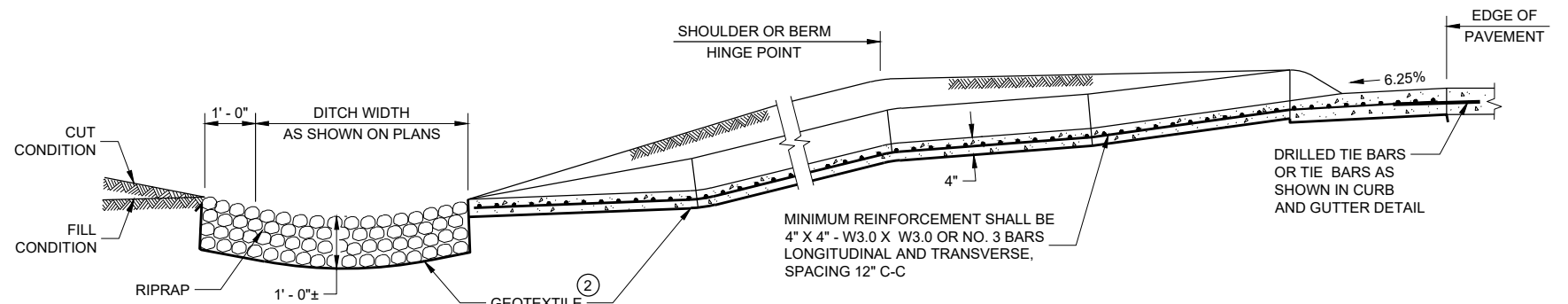
SECTION A - A



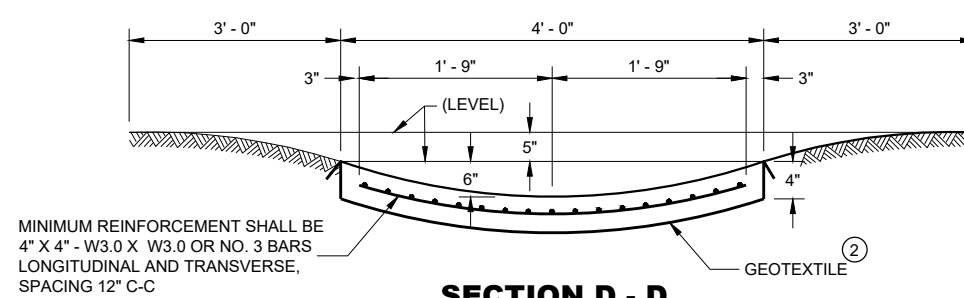
SECTION B - B



**PLAN VIEW
CONCRETE SURFACE DRAIN**



SECTION C - C



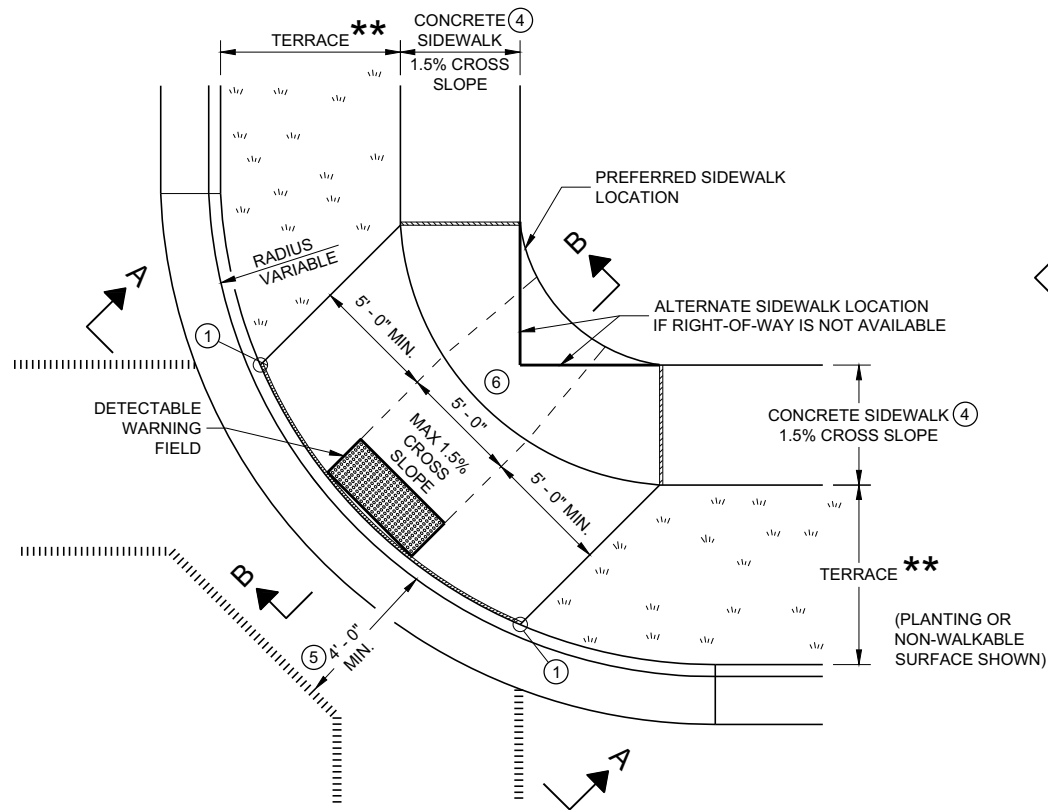
SECTION D - D

CONCRETE SURFACE DRAINS AND ASPHALTIC FLUMES

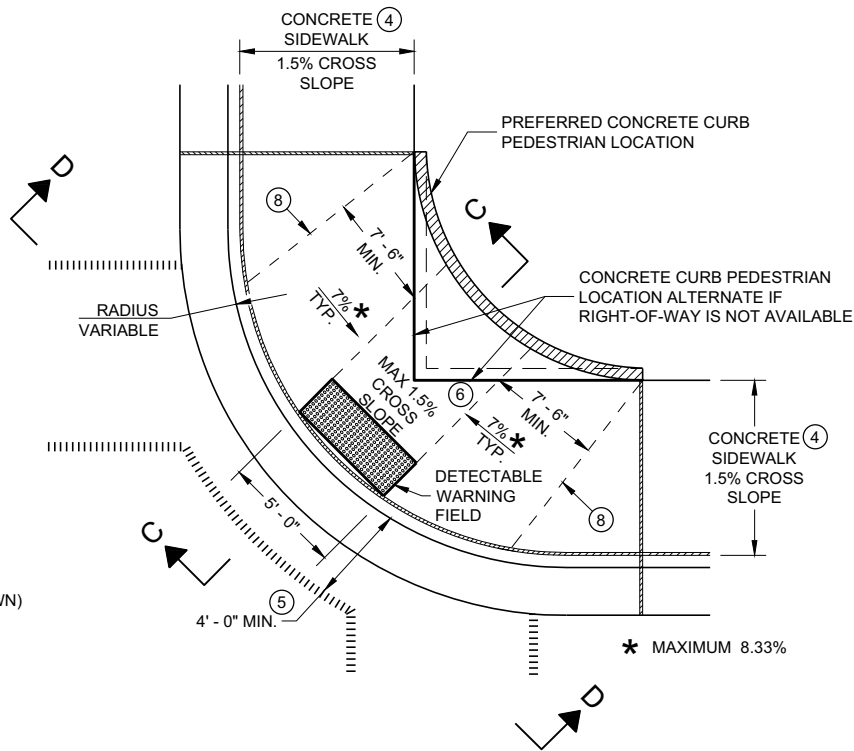
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE May 2023 /S/ Rodney Taylor
ROADWAY STANDARDS DEVELOPMENT
ENGINEER

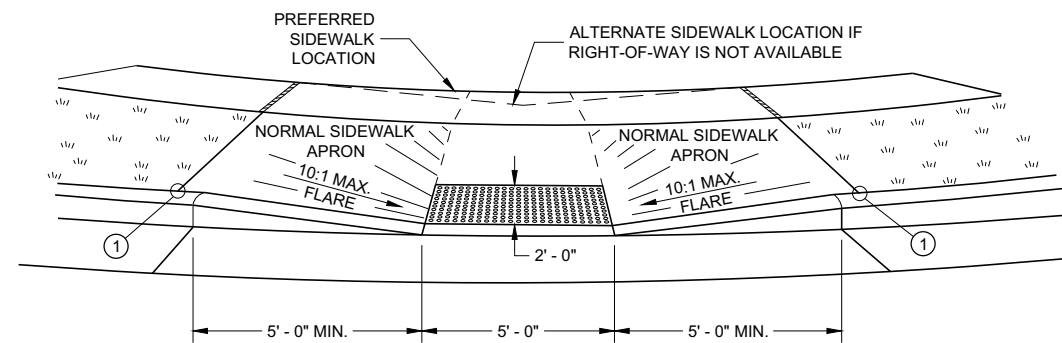
FHWA



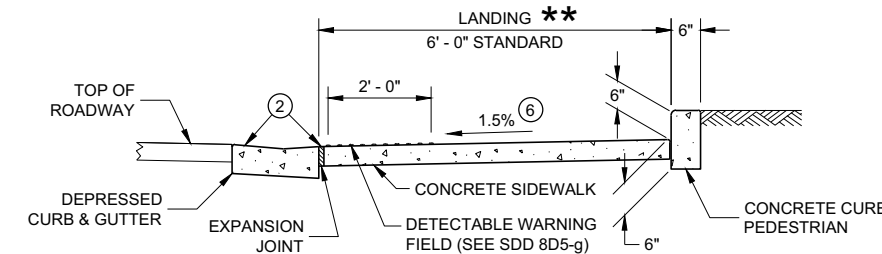
**PLAN VIEW
CURB RAMP TYPE 1
(CENTER OF CORNER RADIUS)**



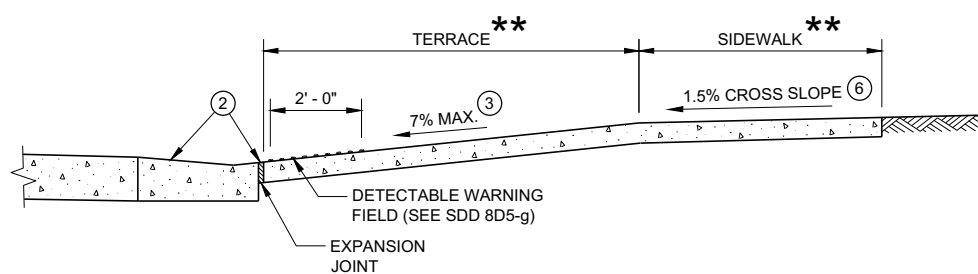
**PLAN VIEW
CURB RAMP TYPE 1 - A
(NO TERRACE)**



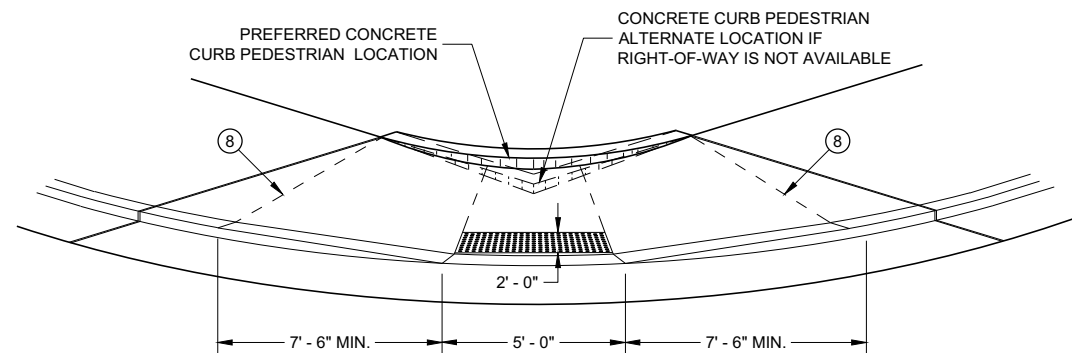
VIEW A - A FOR TYPE 1



SECTION C - C FOR TYPE 1 - A



SECTION B - B FOR TYPE 1



VIEW D - D FOR TYPE 1 - A

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
 DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

WHEN NECESSARY, THE SIDEWALK ELEVATION MAY BE LOWERED TO MEET THE HIGH POINT ON THE RAMP.
 TYPE 1 CURB RAMPS SHALL HAVE A NORMAL SIDEWALK APRON AND CURB ON BOTH SIDES OF RAMP.

DETECTABLE WARNING FIELD SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS "CURB RAMP DETECTABLE WARNING FIELD". THE CONCRETE PEDESTRIAN CURB, IF NEEDED, SHALL BE MEASURED AND PAID BY THE LINEAR FOOT AS "CONCRETE CURB PEDESTRIAN". CONCRETE SIDEWALK IN THE CURB RAMP AREA SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS CONCRETE SIDEWALK, INCLUDING THE AREA UNDER THE DETECTABLE WARNING FIELD.

SELECT CURB RAMP DETECTABLE WARNING FIELD MATERIALS AND DEVICES FROM THE DEPARTMENT'S APPROVED MATERIALS LIST. THE COLOR OF THE DETECTABLE WARNING FIELD IS SPECIFIED ELSEWHERE AND IS INCIDENTAL TO THE BID ITEM OF "CURB RAMP DETECTABLE WARNING FIELD"

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

SURFACE TEXTURE OF THE RAMP SHALL BE OBTAINED BY COARSE BROOMING TRANSVERSE TO THE SLOPE OF THE RAMP.

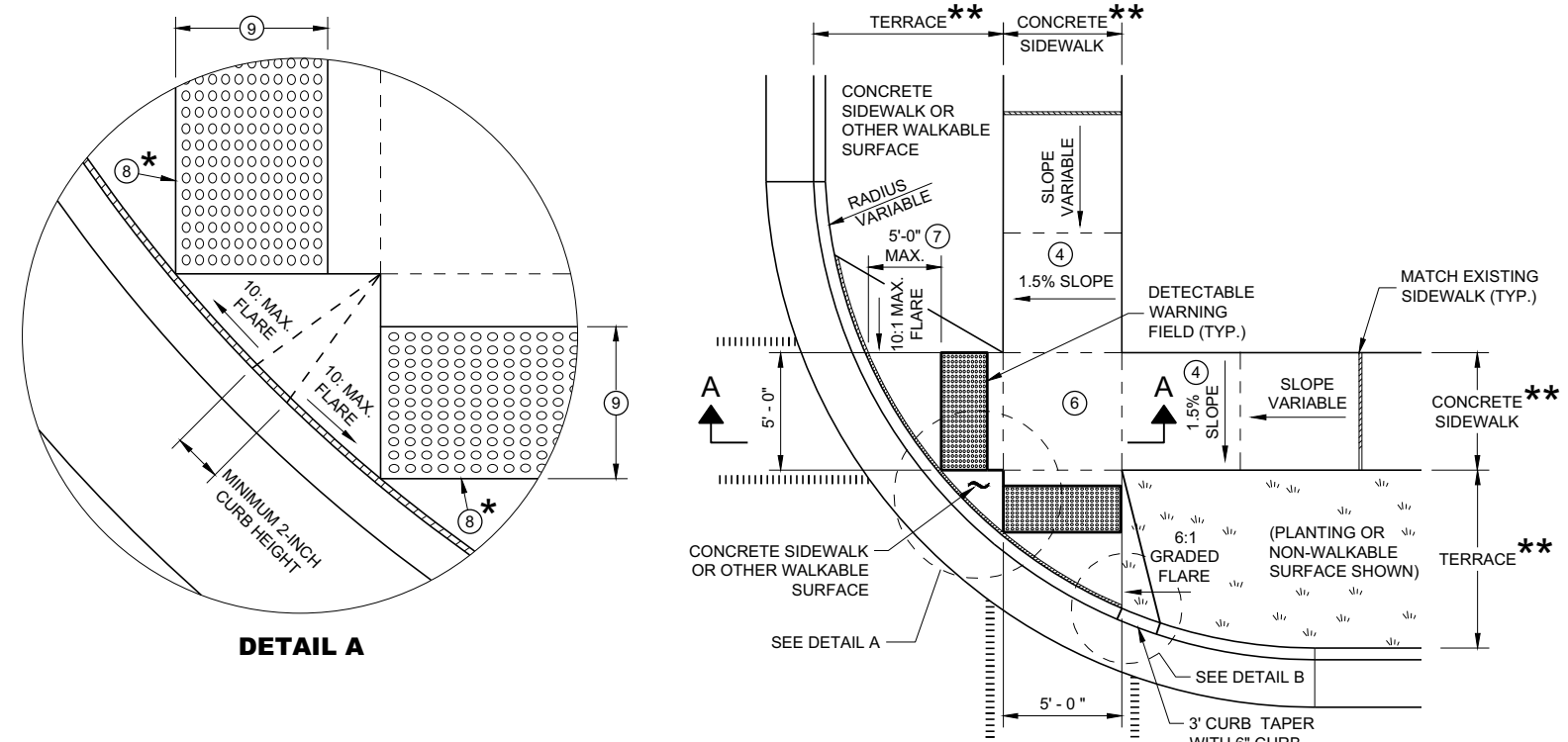
- ① THIS POINT IS AN EXTENSION OF OUTSIDE EDGE OF APPROACHING SIDEWALK WHERE IT MEETS THE BACK OF CONCRETE CURB. POINT LOCATION MAY BE ADJUSTED TO ALIGN WITH BEGINNING OF FULL-HEIGHT CURB IF THIS DISTANCE IS SHORT.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4" INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ MAXIMUM 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑤ PROVIDE A LEVEL LANDING IN THE STREET AND GUTTER AREA (2% MAXIMUM SLOPE IN ANY DIRECTION). WHEN THE GUTTER SLOPE EXCEEDS 2%, CONSTRUCT THE LEVEL LANDING IN THE STREET AREA.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

LEGEND

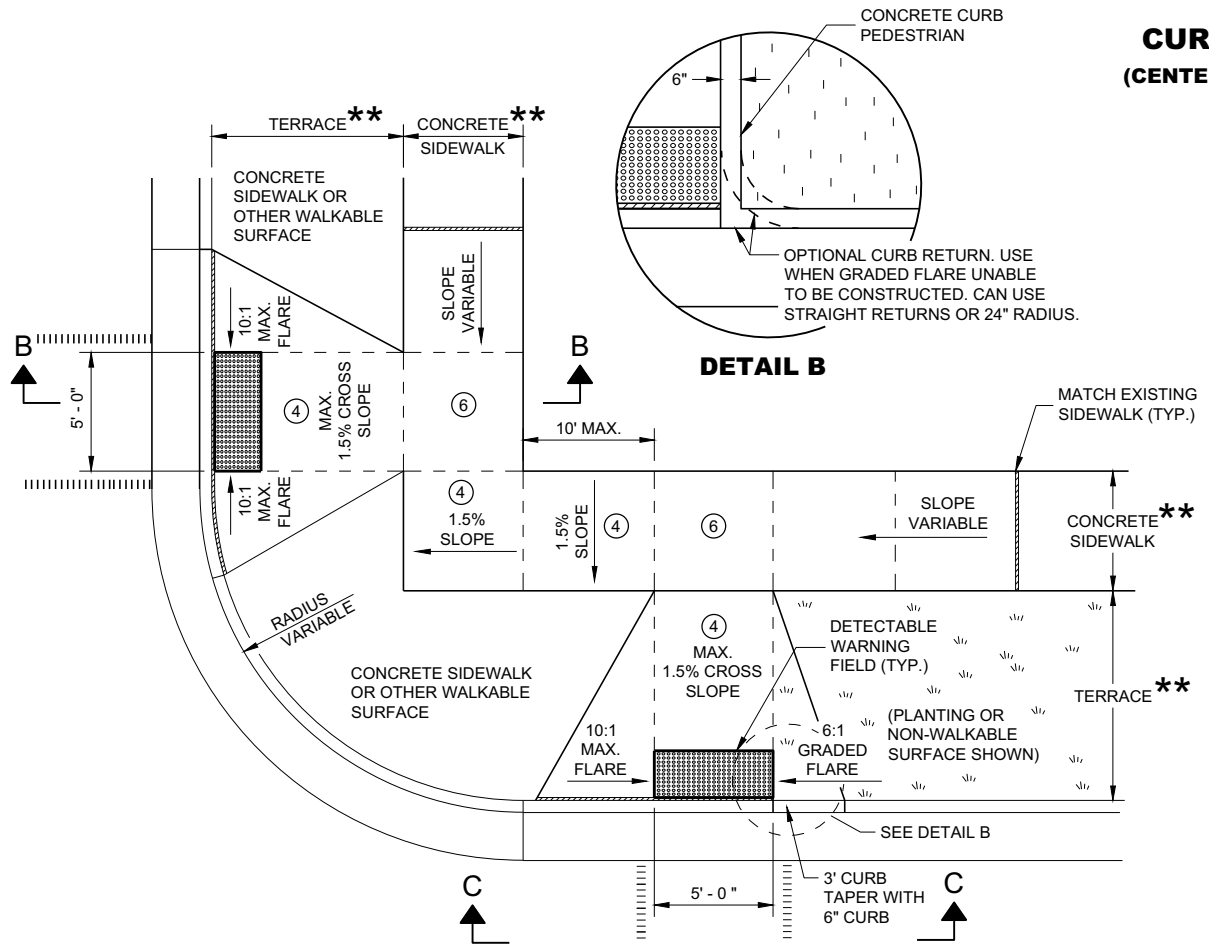
- 1/2" EXPANSION JOINT SIDEWALK
- - - - - CONTRACTION JOINT FIELD LOCATED
- ||||||| PAVEMENT MARKING CROSSWALK (WHITE)

**CURB RAMPS
TYPE 1 AND 1-A**

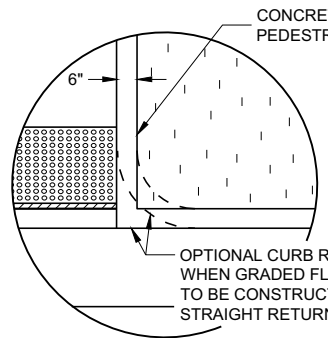
STATE OF WISCONSIN
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PLAN VIEW CURB RAMP TYPE 2 (CENTER OF CORNER RADIUS)



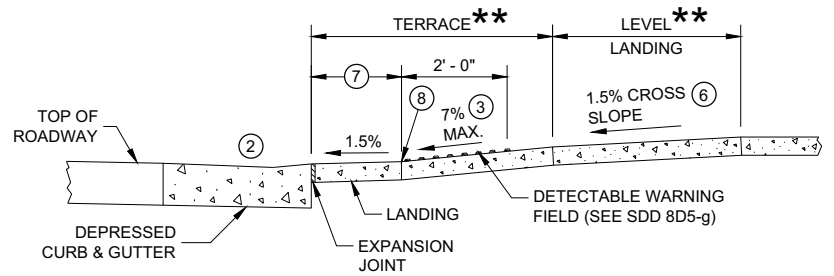
PLAN VIEW CURB RAMP TYPE 3 (OUTSIDE OF CROSSWALK AREA)



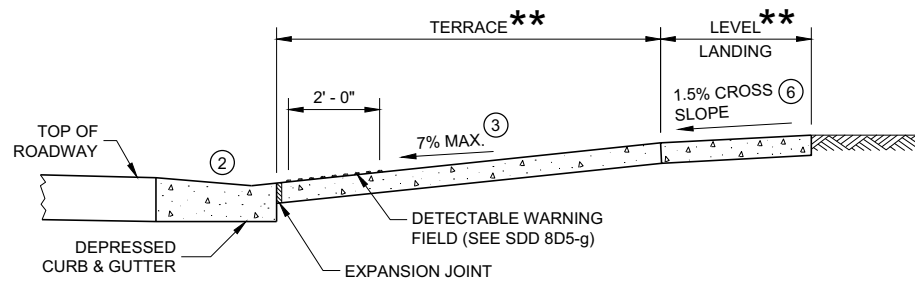
DETAIL B

GENERAL NOTES

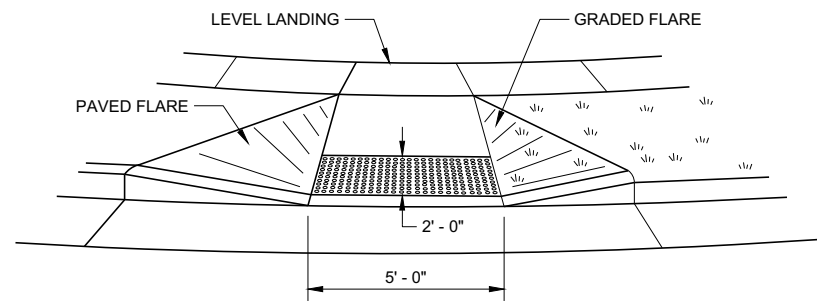
- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4" INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE (2.67% OR LESS) AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET X 5 FEET.
- ⑦ WHEN GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑨ WHEN DISTANCE IS LESS THAN 6' - 0", IT MAY BE DIFFICULT TO ACHIEVE A 7% SLOPE OR FLATTER ALONG THE RAMP. REDUCE CURB HEIGHT IN TRIANGLE AREA TO ACHIEVE 7% SLOPE OR FLATTER ON RAMP. CONSTRUCT 2-INCH MINIMUM CURB HEIGHT BETWEEN 10:1 FLARES.



SECTION A - A FOR TYPE 2



SECTION B - B FOR TYPE 3



VIEW C - C FOR TYPE 3

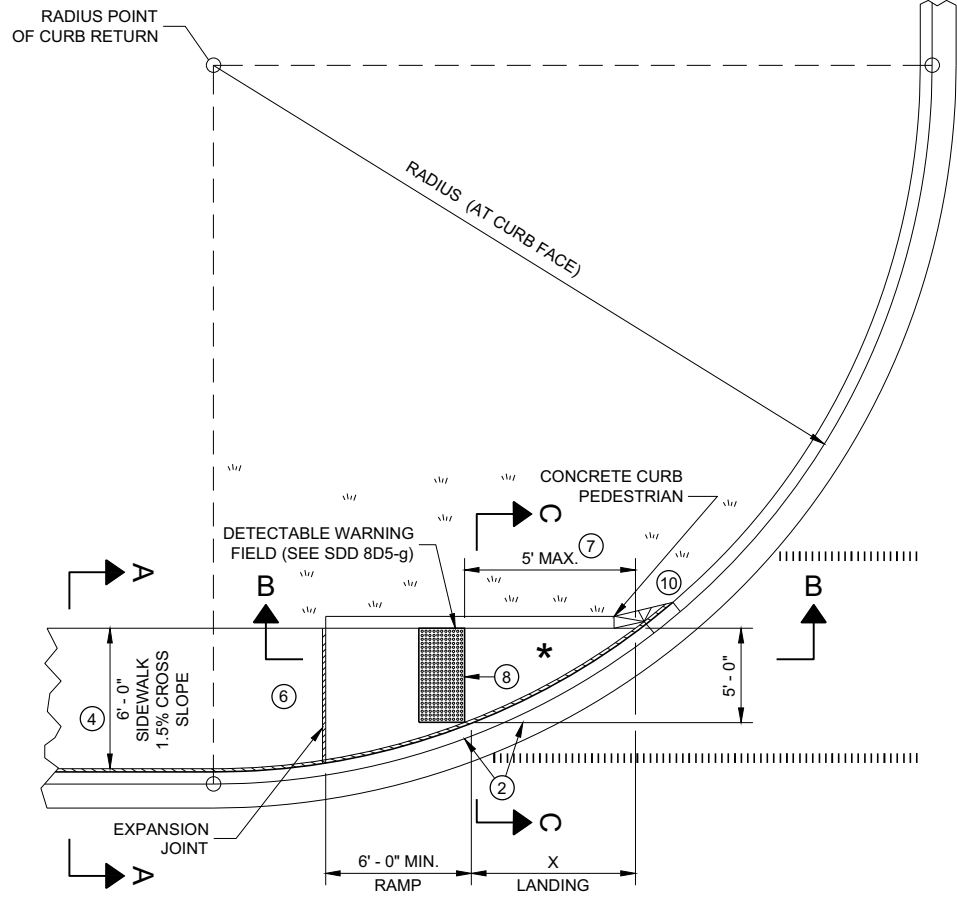
- * MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK
- ** WIDTH SHOWN ELSEWHERE IN THE PLANS

LEGEND

- 1/2" EXPANSION JOINT SIDEWALK
- - - CONTRACTION JOINT SIDEWALK
- ||||| PAVEMENT MARKING CROSSWALK (WHITE)

**CURB RAMPS
TYPE 2 AND 3**

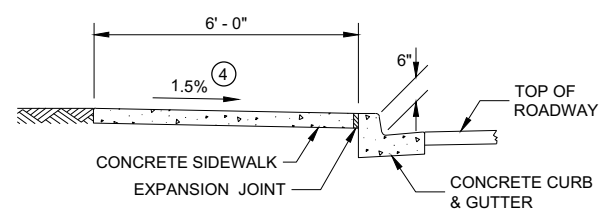
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**PLAN VIEW
CURB RAMP TYPE 4A**

RADIUS (AT CURB FACE)	X
10 FEET	4' - 7"
15 FEET	6' - 5 1/2"

INTERMEDIATE RADII CAN BE INTERPOLATED



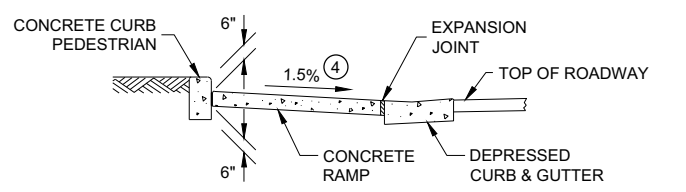
SECTION A - A FOR TYPE 4A

GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑦ WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑩ INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

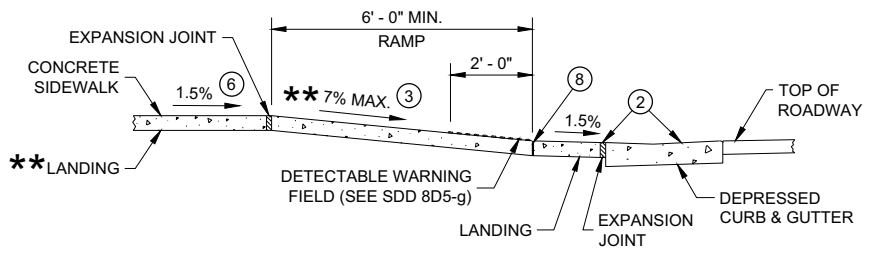
LEGEND

- 1/2" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT SIDEWALK
- PAVEMENT MARKING CROSSWALK (WHITE)



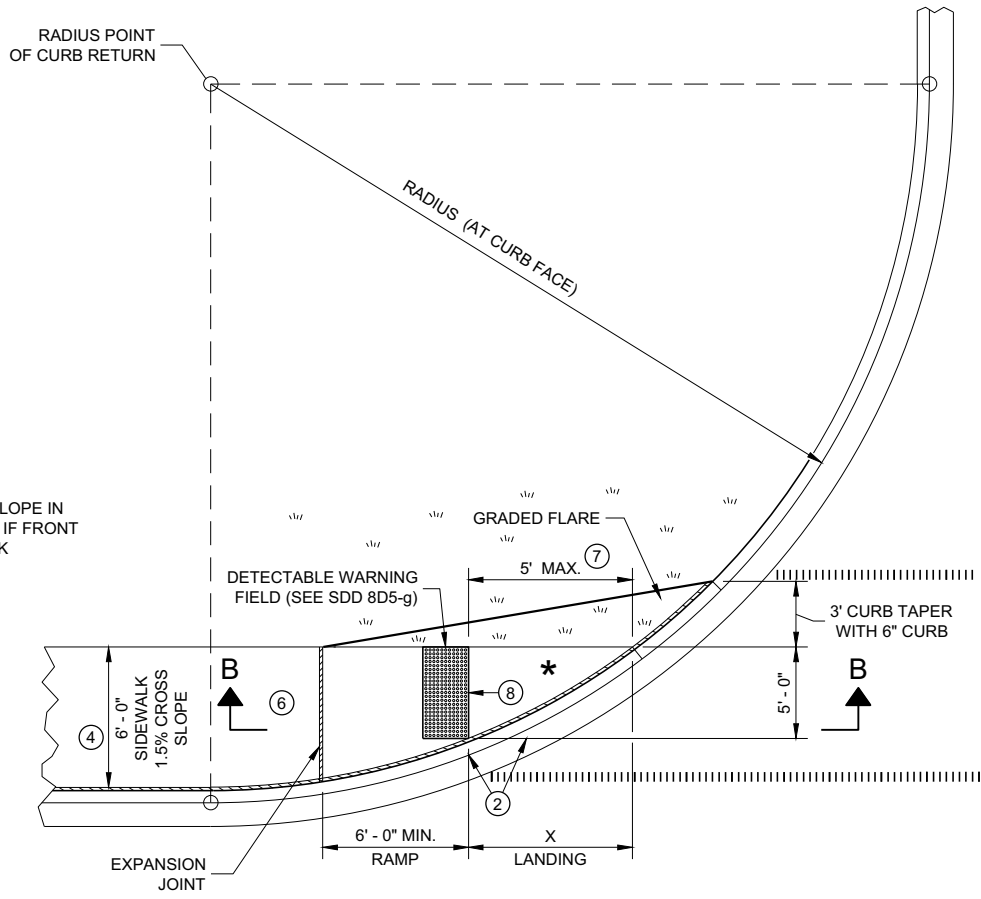
SECTION C - C FOR TYPE 4A

* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IF FRONT OF GRADE BREAK

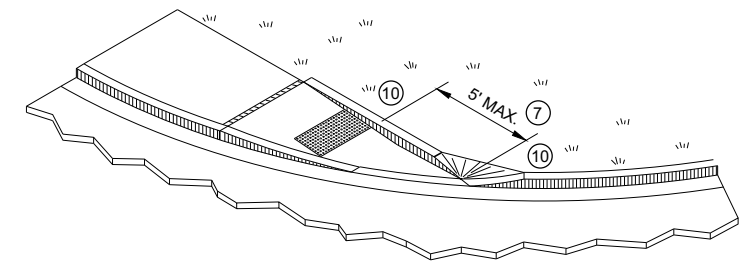


**SECTION B - B FOR
TYPE 4A AND TYPE 4A1**

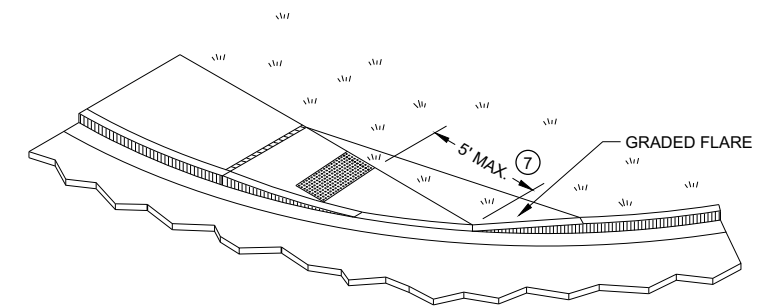
** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED



**PLAN VIEW
CURB RAMP TYPE 4A1**



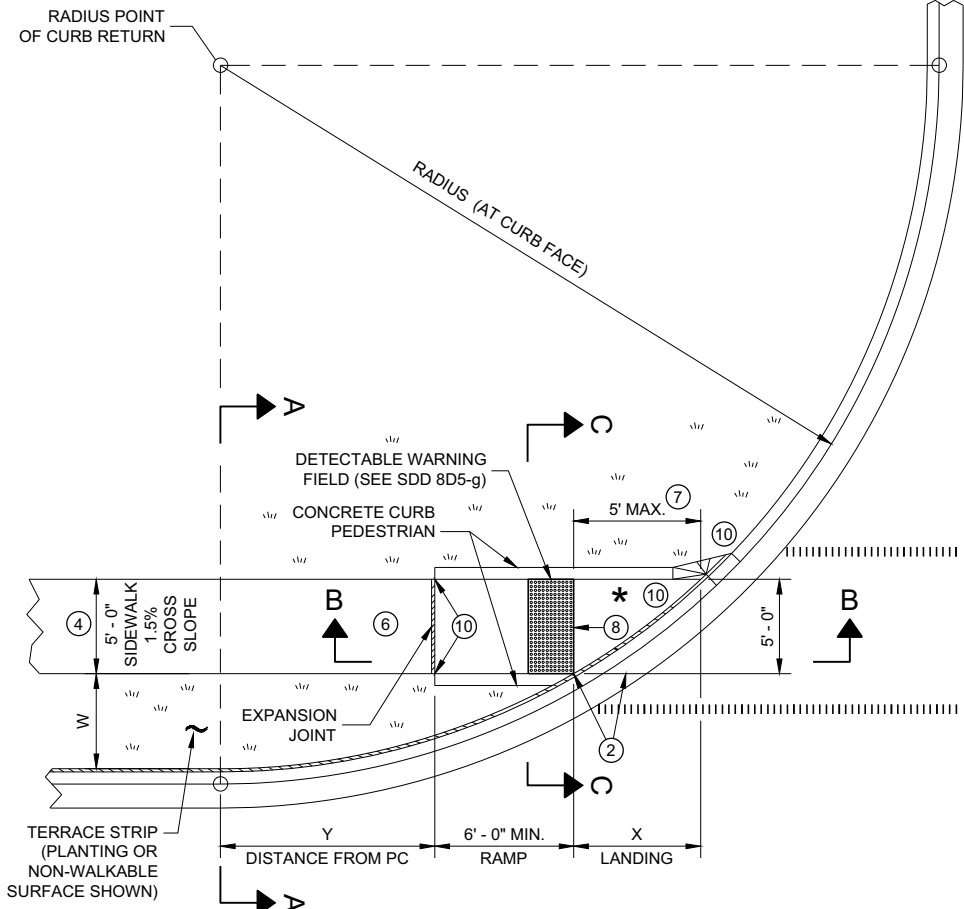
ISOMETRIC VIEW FOR TYPE 4A



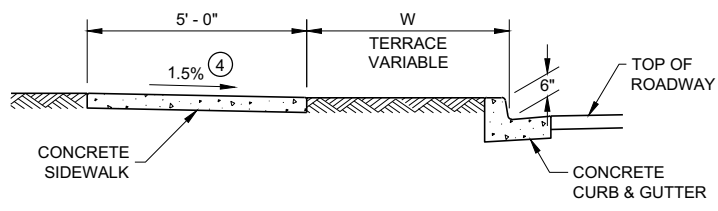
ISOMETRIC VIEW FOR TYPE 4A1

**CURB RAMPS
TYPE 4A AND 4A1**

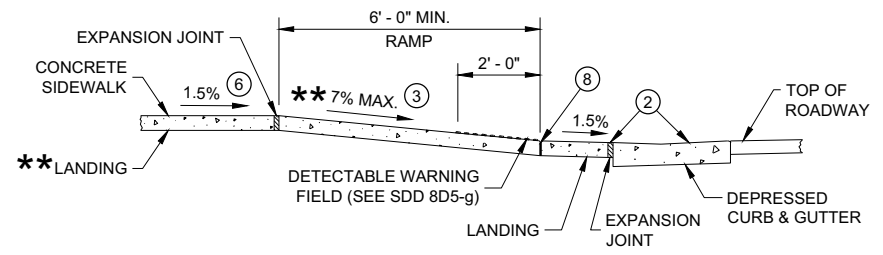
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**PLAN VIEW
CURB RAMP TYPE 4B**



SECTION A - A FOR TYPE 4B



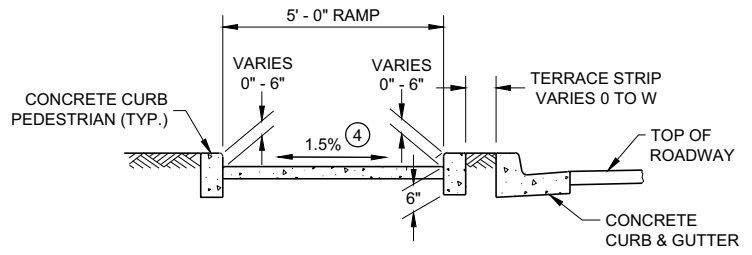
**SECTION B - B FOR
TYPE 4B AND TYPE 4B1**

** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

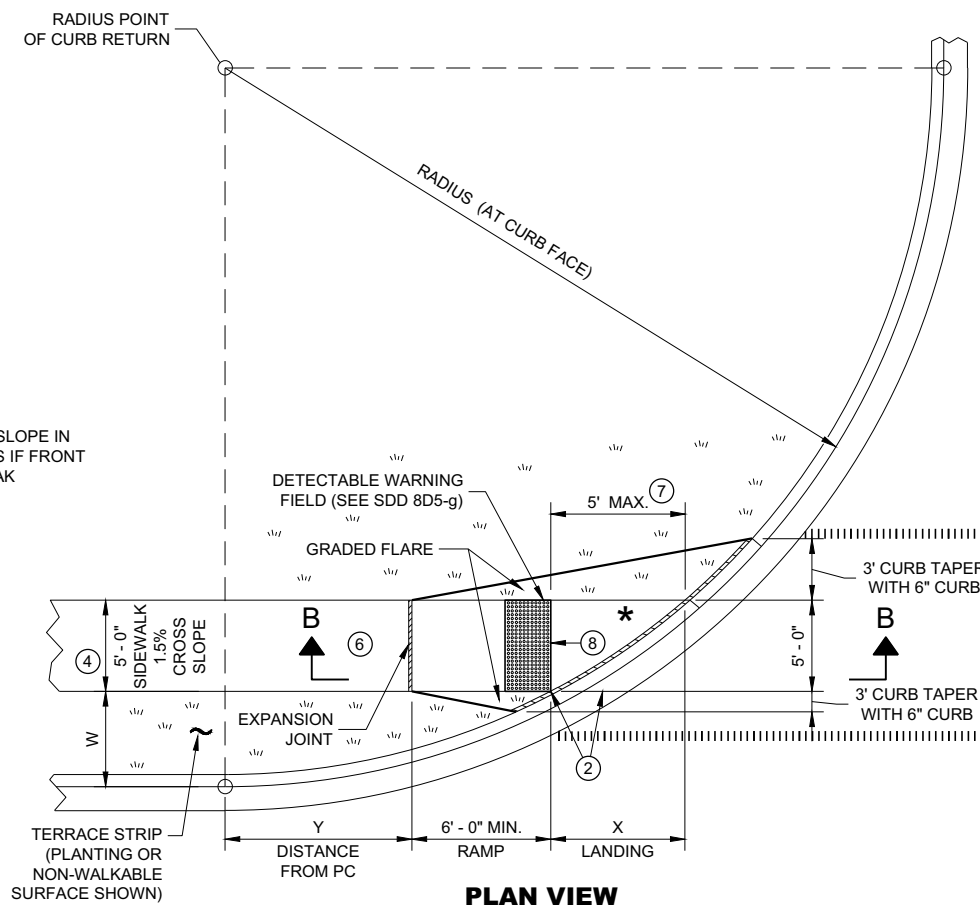
* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IF FRONT OF GRADE BREAK

RADIUS (AT CURB FACE)	W = 3' - 0"		W = 4' - 0"		W = 5' - 0"		W = 6' - 0"		W = 7' - 0"		W = 8' - 0"		W = 9' - 0"		W = 10' - 0"	
	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y
10 FEET	2' - 10 1/4"	0' - 5"	2' - 1"	1' - 4 1/2"	1' - 5"	2' - 1"	0' - 10"	2' - 7 1/2"	0' - 3 1/4"	3' - 0 1/4"						
15 FEET	4' - 6 3/4"	2' - 1 3/4"	3' - 9"	3' - 5 3/4"	3' - 1 1/4"	4' - 6"	2' - 6 3/4"	5' - 4 1/2"	2' - 1"	6' - 1"	1' - 8"	6' - 8 1/2"	1' - 3 1/4"	7' - 2 1/2"	0' - 10 3/4"	7' - 7 1/4"
20 FEET	5' - 9 3/4"	3' - 6 1/2"	4' - 11 1/2"	5' - 1 3/4"	4' - 3 1/4"	6' - 5 1/2"	3' - 8 3/4"	7' - 7"	3' - 3"	8' - 6 1/2"	2' - 10"	9' - 4 1/2"	2' - 5 1/2"	10' - 1 1/4"	2' - 1 1/4"	10' - 9"
30 FEET			6' - 9 1/4"	7' - 11 1/4"	6' - 0 1/4"	9' - 8"	5' - 5"	11' - 1 3/4"	4' - 10 3/4"	12' - 5 3/4"	4' - 5 1/2"	13' - 7 3/4"	4' - 0 3/4"	14' - 8 1/2"	3' - 8 1/2"	15' - 8 1/4"
40 FEET									6' - 1 3/4"	15' - 8 1/2"	5' - 8"	17' - 2"	5' - 3"	18' - 5 3/4"	4' - 10 3/4"	19' - 8 1/4"
50 FEET															5' - 10 1/4"	23' - 2"

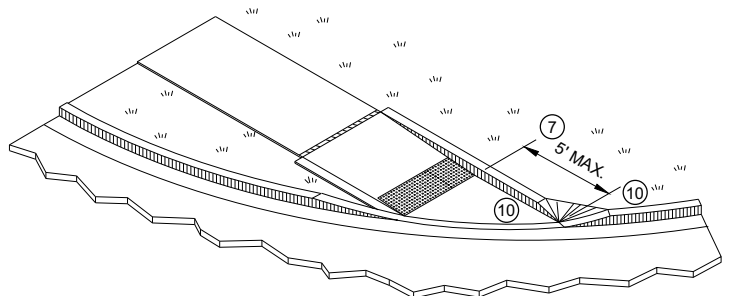
INTERMEDIATE RADII CAN BE INTERPOLATED
DIMENSION "Y" IS CALCULATED BASED ON 6'-0" RAMP LENGTH
DIMENSION "X" IS CALCULATED BASED ON 5'-0" SIDEWALK WIDTH



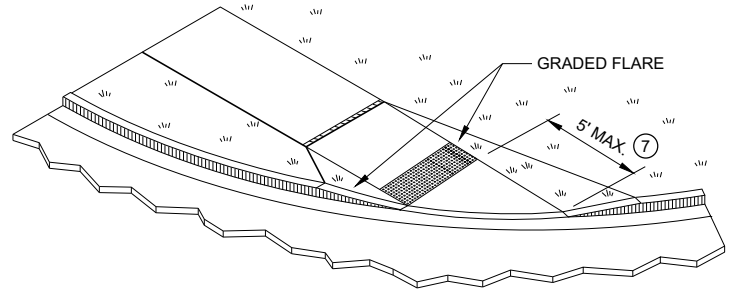
SECTION C - C FOR TYPE 4B



**PLAN VIEW
CURB RAMP TYPE 4B1**



ISOMETRIC VIEW FOR TYPE 4B



ISOMETRIC VIEW FOR TYPE 4B1

LEGEND

- 1/2" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT SIDEWALK
- PAVEMENT MARKING CROSSWALK (WHITE)

GENERAL NOTES

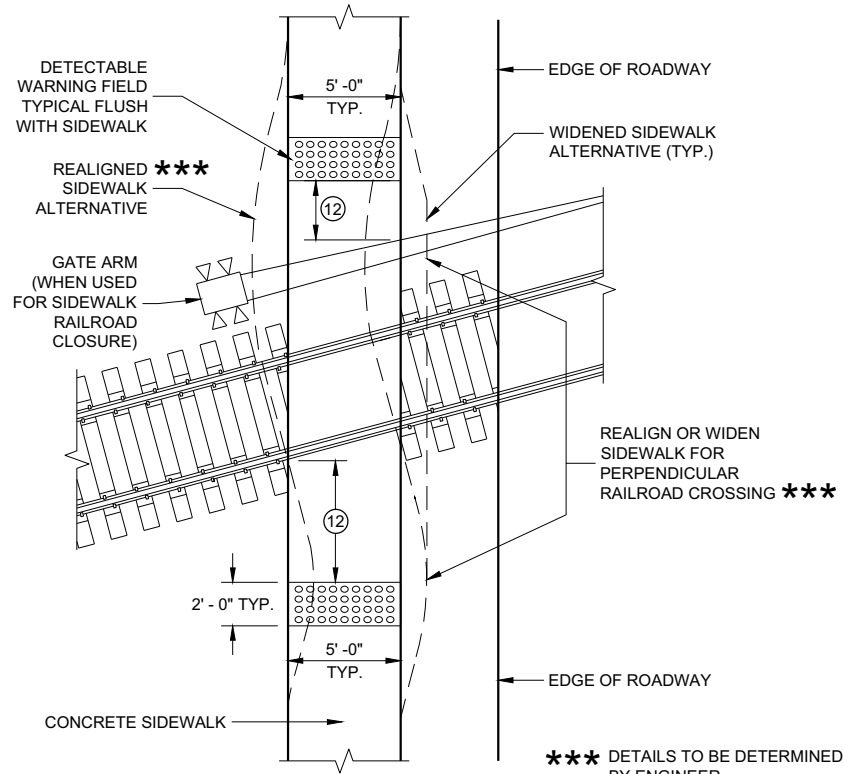
- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/2 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑦ WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑩ INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

SDD08D05 - 20d

SDD08D05 - 20d

**CURB RAMPS
TYPE 4B AND 4B1**

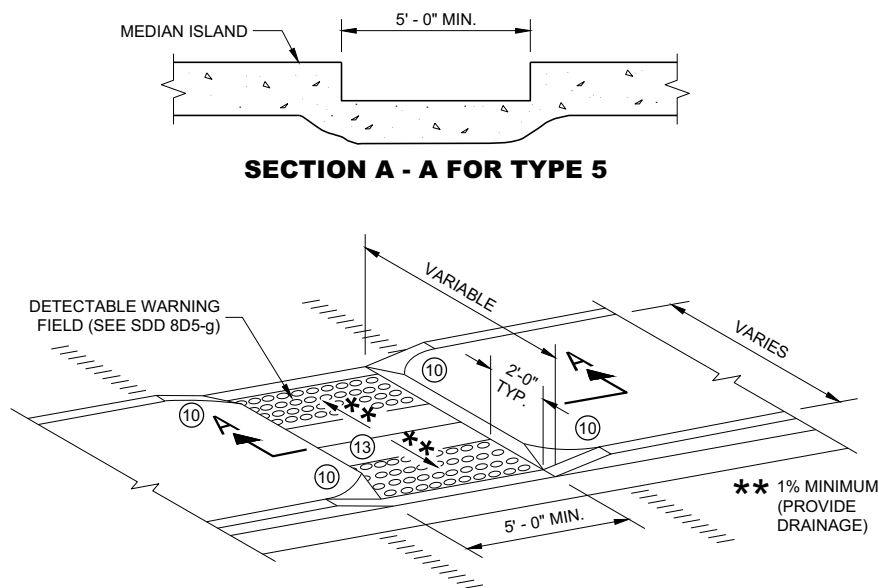
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CURB RAMP TYPE 8

DETECTABLE WARNINGS AT RAILROAD CROSSING

*** DETAILS TO BE DETERMINED BY ENGINEER



CURB RAMP TYPE 5
MEDIAN ISLAND
NON-ELEVATED PEDESTRIAN CROSSING

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

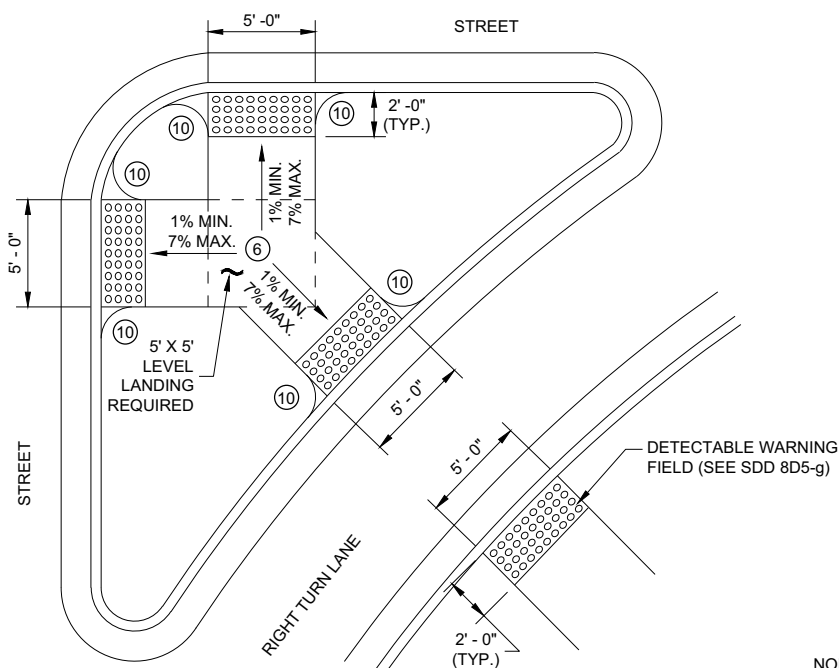
SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑩ INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.
- ⑪ SLOPE SIDEWALK TOWARD LANDING AS SHOWN WHERE THERE IS NO TERRACE OR WHERE THE TERRACE WIDTH IS LESS THAN 6 FEET WIDE.
- ⑫ THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO A RAILROAD CROSSING SHALL BE 1.5 FEET ±0.1' FROM THE FACE OF THE GATE ARM IF THE GATE ARM EXTENDS ACROSS THE SIDEWALK. WHERE THERE IS NO PEDESTRIAN GATE, THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO THE RAILROAD CROSSING SHALL BE 15 FEET FROM THE NEAREST RAIL.
- ⑬ DO NOT INSTALL DETECTABLE WARNING FIELDS AT THE EDGES OF STREET-LEVEL PEDESTRIAN REFUGE ISLANDS IF A MINIMUM 2 FOOT CONCRETE SURFACE WITHOUT DETECTABLE WARNINGS (MEASURED IN THE DIRECTION OF PEDESTRIAN TRAVEL) CANNOT BE ACHIEVED.

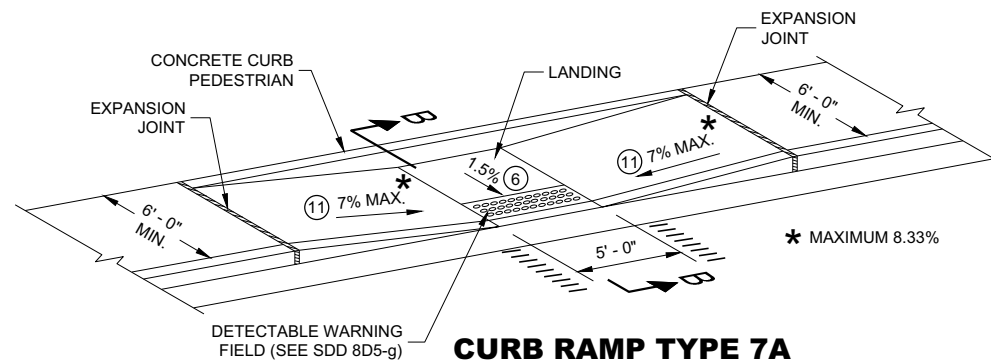
LEGEND

- ===== 1/2" EXPANSION JOINT SIDEWALK
- - - - - CONTRACTION JOINT FIELD LOCATED
- ||||||| PAVEMENT MARKING CROSSWALK (WHITE)

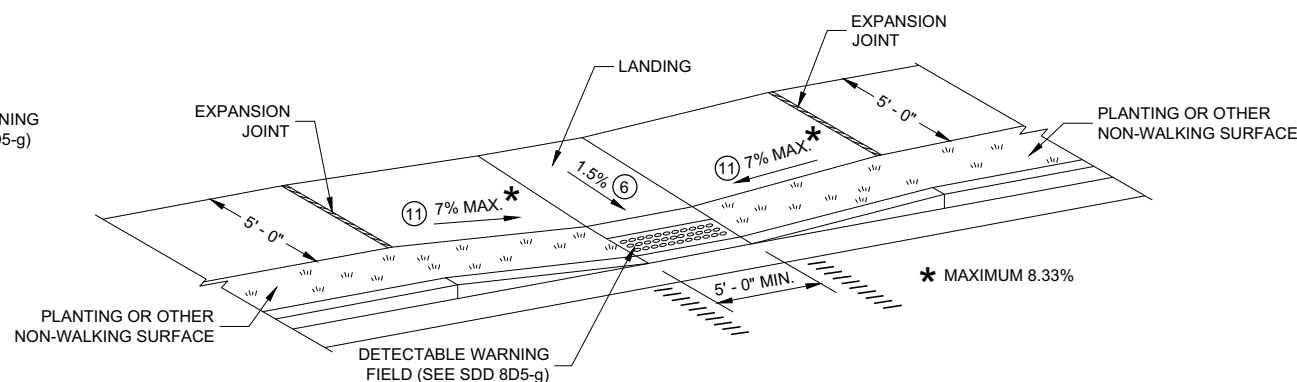


CURB RAMP TYPE 6
DETECTABLE WARNING AT ISLANDS

REFER TO GENERAL NOTES ② AND ③ FOR ALL ISLAND CURB RAMPS

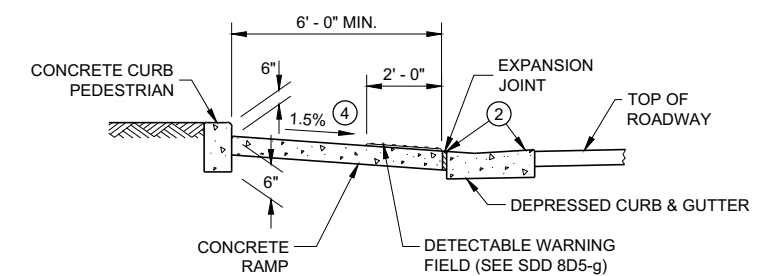


CURB RAMP TYPE 7A
MID BLOCK CROSSING



CURB RAMP TYPE 7B
MID BLOCK CROSSING

NOTE: THESE PARALLEL AND PARALLEL/PERPENDICULAR CURB RAMPS MAY BE USED AT INTERSECTIONS AND MID BLOCK LOCATIONS.

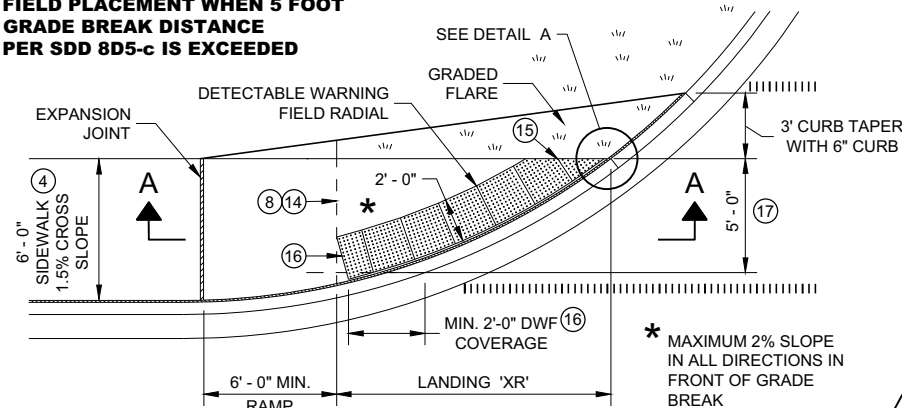


SECTION B - B FOR TYPE 7A

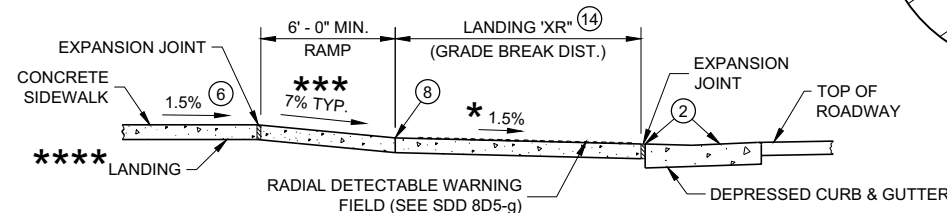
CURB RAMPS
TYPE 5, 6, 7A, 7B & 8

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-c IS EXCEEDED



PLAN VIEW CURB RAMP TYPE 4A1 (GRADE BREAK DISTANCE GREATER THAN 5 FEET)

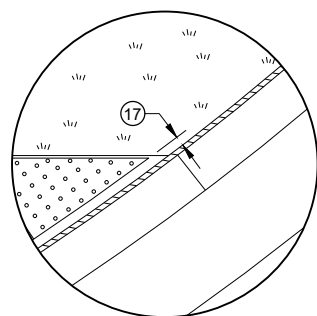


SECTION A - A FOR TYPE 4A1

**** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

*** MAXIMUM 8.33%

- LEGEND**
- 1/2" EXPANSION JOINT SIDEWALK
 - - - CONTRACTION JOINT SIDEWALK
 - ||||| PAVEMENT MARKING CROSSWALK (WHITE)

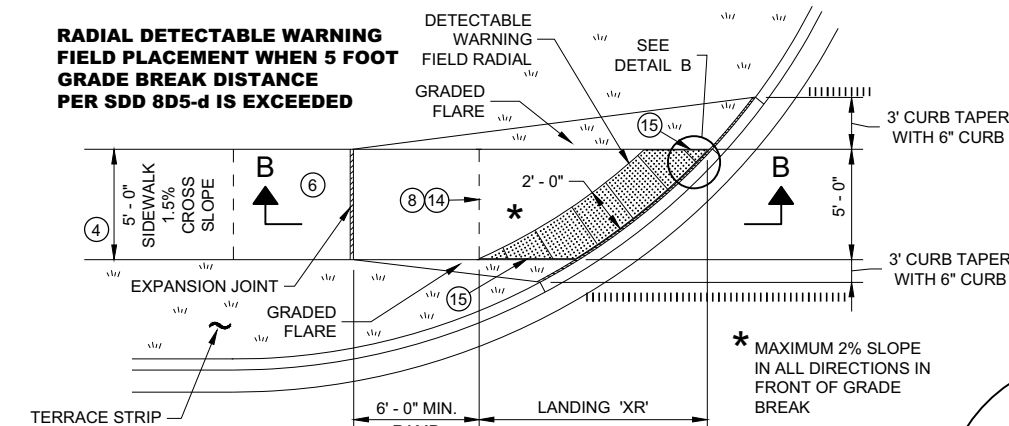


DETAIL A

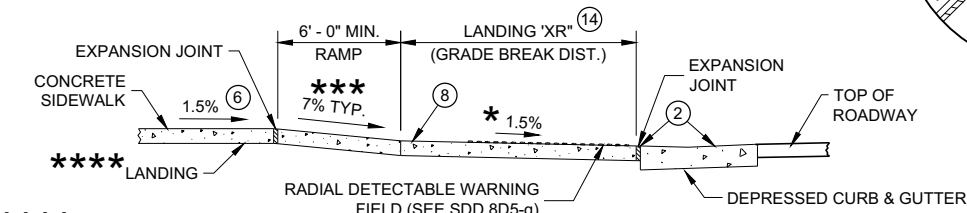
GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- APPLY RADIAL DETECTABLE WARNING PLACEMENT SIMILARLY FOR TYPE 4A AND 4A1 CURB RAMPS AND SIMILARLY FOR TYPE 4B AND 4B1 CURB RAMPS. TYPE 4A AND 4B RAMPS ARE NOT SHOWN.
- REFER TO SDD 8D5-g FOR ADDITIONAL RADIAL PLATE REQUIREMENTS.
- FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FILED ARE PROHIBITED.
- DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.
- 2 GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- 3 AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- 4 ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- 6 PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET BY 5 FEET.
- 8 PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- 14 CONSULT ENGINEER IF GRADE BREAK LOCATION (END OF LANDING DIMENSION "XR") REQUIRES FIELD ADJUSTMENT WHEN ESTABLISHING FINAL RADIAL DETECTABLE WARNING FIELD LOCATION.
- 15 FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN 1/8" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.
- 16 USE 1' X 2" RECTANGULAR END PLATE AT END OF TYPE 4A1 RAMP AND PROVIDE MINIMUM 2' - 0" DETECTABLE WARNING FIELD COVERAGE (IN DIRECTION OF PEDESTRIAN TRAVEL) ALONG THE ENTIRE CURB RAMP WIDTH.
- 17 A MAXIMUM 3 INCH CONCRETE BORDER WITH IS ALLOWABLE IN FROM OF RADIAL DETECTABLE WARNING FIELD FOR CONSTRUCTABILITY PURPOSES. CONCRETE BORDER WIDTH MAY VARY UP TO 1 INCH.

RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-d IS EXCEEDED



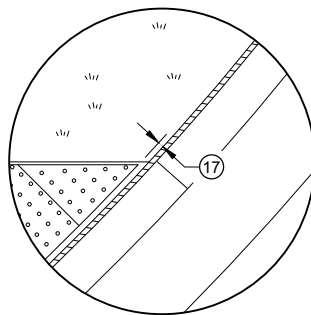
PLAN VIEW CURB RAMP TYPE 4B1 (GRADE BREAK DISTANCE GREATER THAN 5 FEET)



SECTION B - B FOR TYPE 4B1

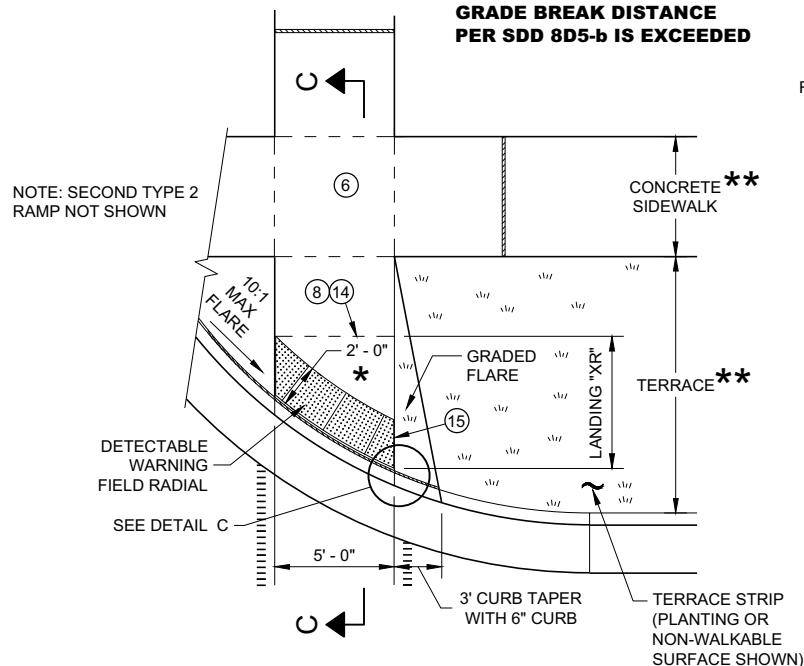
**** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

*** MAXIMUM 8.33%



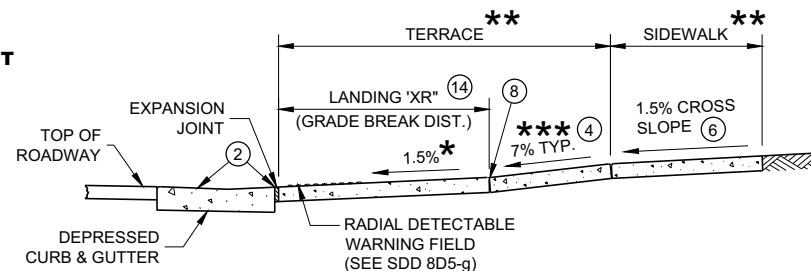
DETAIL B

RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-b IS EXCEEDED



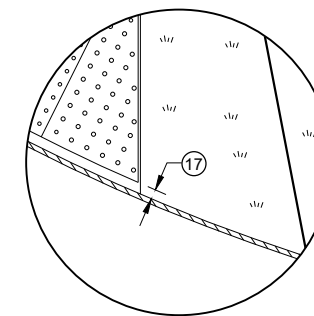
PLAN VIEW CURB RAMP TYPE 2 (GRADE BREAK DISTANCE GREATER THAN 5 FEET) (ON LINE WITH SIDEWALK)

NOTE: SECOND TYPE 2 RAMP NOT SHOWN



SECTION C - C FOR TYPE 2

- * MAXIMUM 2% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK
- ** WIDTH SHOWN ELSEWHERE IN THE PLANS
- *** MAXIMUM 8.33%



DETAIL C

CURB RAMPS RADIAL DETECTABLE WARNING FIELD APPLICATIONS

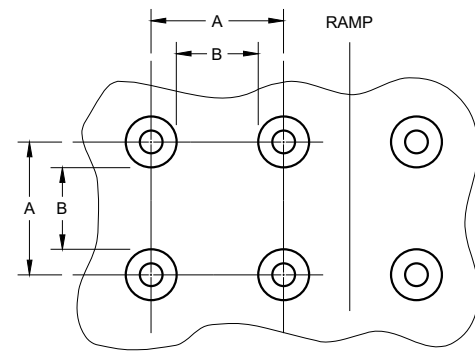
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

6

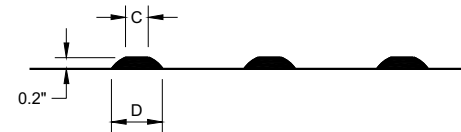
6

	MIN.	MAX.
A	1.6"	2.4"
B	0.65"	1.5"
C	*	*
D	0.9"	1.4"

* THE C DIMENSION IS 50% TO 65% OF THE D DIMENSION.

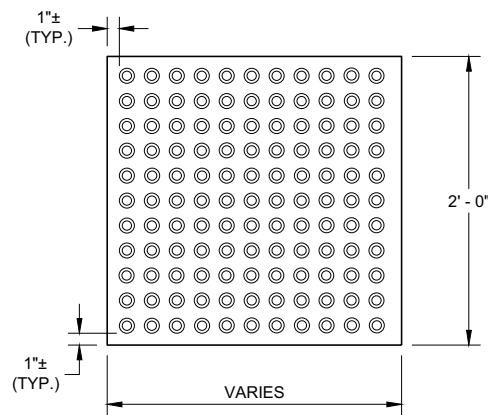


PLAN VIEW

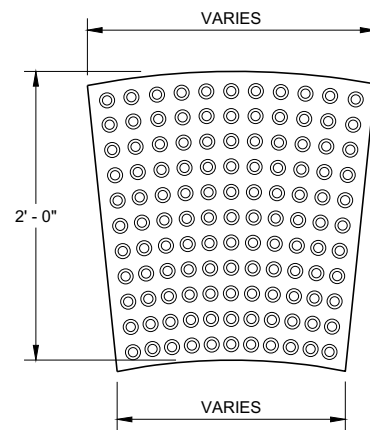


ELEVATION VIEW

**TRUNCATED DOMES
DETECTABLE WARNING PATTERN DETAIL**

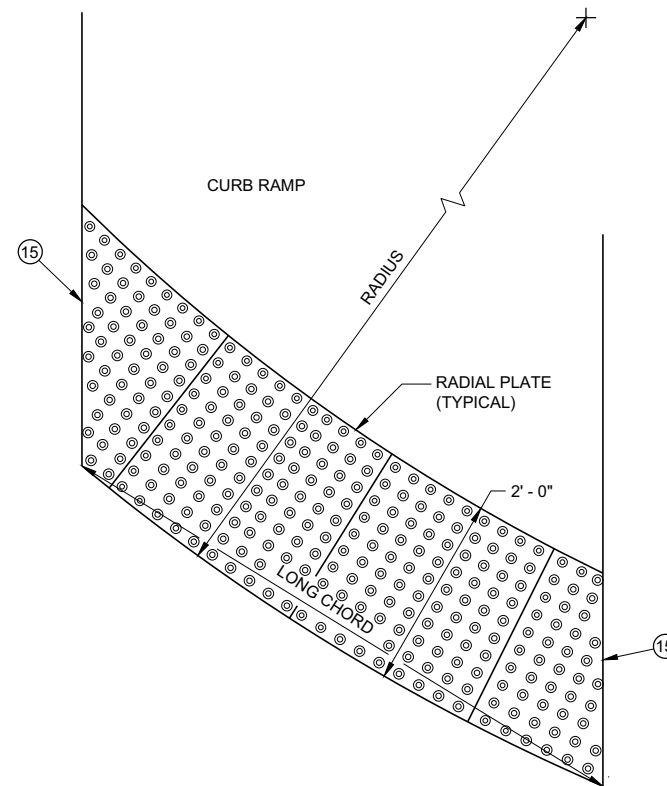


**RECTANGULAR
PLATES**

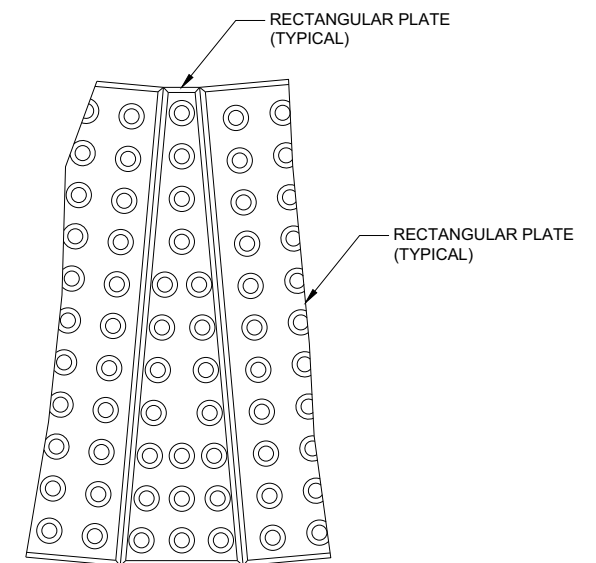


**RADIAL
PLATES**

**PLAN VIEW
DETECTABLE WARNING FIELDS (TYPICAL)**



**PLAN VIEW
RADIAL DETECTABLE
WARNING FIELD ATTRIBUTES**



**PLAN VIEW
RADIAL WEDGE PLATE
CONNECTION DETAIL**

GENERAL NOTES

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AT A CURB RAMP SHALL BE FROM THE SAME MANUFACTURER.

PLACE ALL DETECTABLE WARNING FIELD SYSTEMS IN ACCORDANCE TO THE MANUFACTURER'S RECOMMENDATION.

FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FIELD ARE PROHIBITED.

DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.

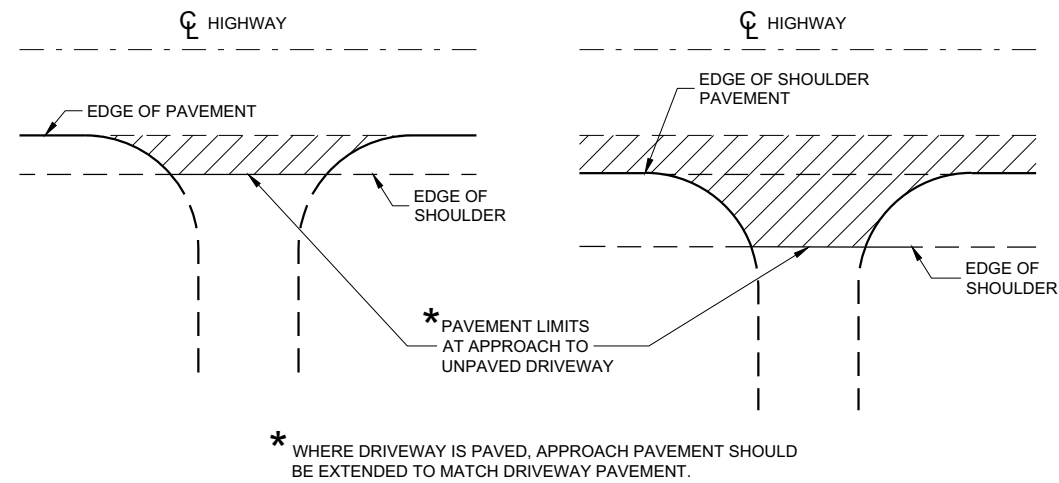
FOR RADIAL DETECTABLE WARNING FIELD APPLICATIONS WHERE STANDARD RADIAL PLATES ARE NOT AVAILABLE AT AN INTERSECTION CURB RADIUS, A COMBINATION OF SQUARE OR RECTANGULAR PLATES AND RADIAL PLATES MAY BE USED TO FORM RADIAL CONFIGURATION. RADIAL WEDGE PLATES IN COMBINATION WITH SQUARE PLATES ARE ALSO ACCEPTABLE. FOLLOW MANUFACTURER'S RECOMMENDATIONS.

REFER TO CONTRACT AND STANDARD SPECIFICATIONS FOR FIELD CUTTING REQUIREMENTS.

DO NOT EMBED IN CONCRETE ANY FIELD-CUT PLATES WITH CUT EDGES SHORTER THAN 6 INCHES. CONSULT WITH MANUFACTURER FOR RE-DRILLING AND ANCHORING REQUIREMENTS OF FIELD-CUT PLATES.

15 FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN 1/8" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.

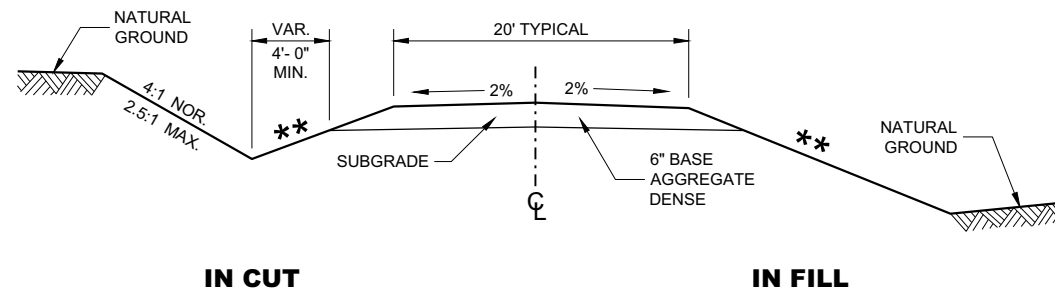
CURB RAMPS RECTANGULAR AND RADIAL DETECTABLE WARNING PLATES	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2019 DATE	/S/ Rodney Taylor ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR
<small>FHWA</small>	



PLAN VIEW
(UNPAVED SHOULDER ON HIGHWAY)

PLAN VIEW
(PAVED SHOULDER ON HIGHWAY)

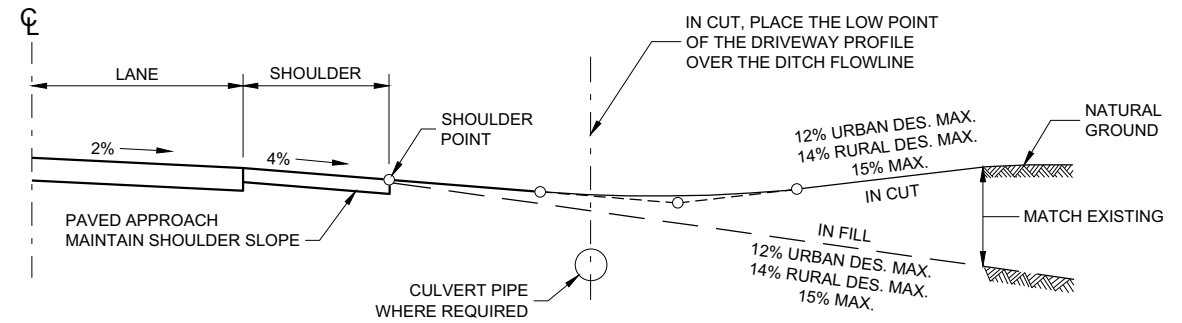
**RURAL DRIVEWAY INTERSECTION DETAIL
(NO CURB AND GUTTER OR SIDEWALK)**



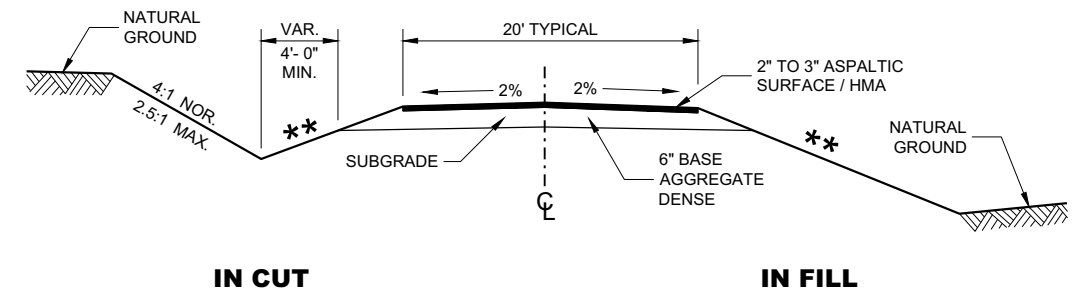
**TYPICAL CROSS SECTION FOR
PRIVATE DRIVE OR FIELD ENTRANCE
AGGREGATE SURFACE**

** SLOPE CAN VARY WITH SPEED. SEE 11-45-30.6.2

POSTED SPEED MPH	MAX. SLOPE
<35	4:1
≥ 35 TO < 60	6:1
≥60	10:1



TYPICAL DRIVEWAY PROFILES



**TYPICAL CROSS SECTION FOR
PRIVATE DRIVE OR FIELD ENTRANCE
ASPHALTIC SURFACE**

DRIVEWAYS WITHOUT CURB AND GUTTER

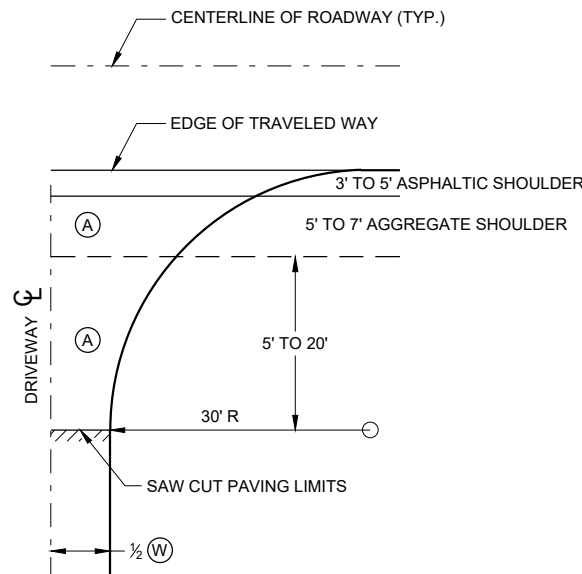
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
December 2017 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR

FHWA

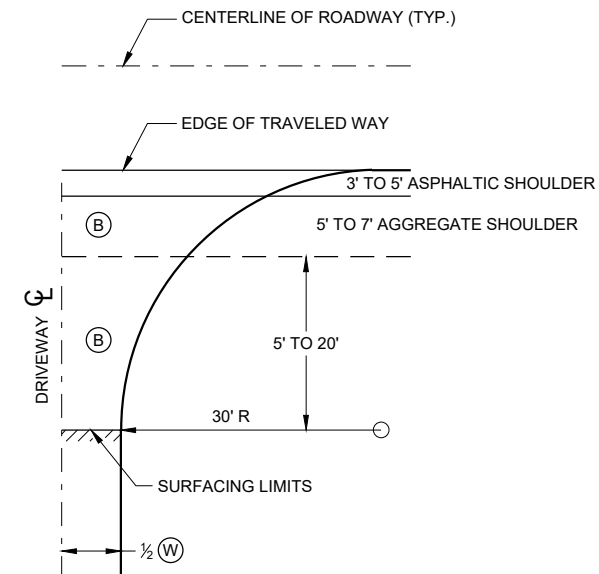
GENERAL NOTES

- ① DESIGN WILL DETERMINE FINAL DRIVEWAY ASPHALTIC THICKNESS BASED ON TYPE OF USAGE AND LOADINGS.

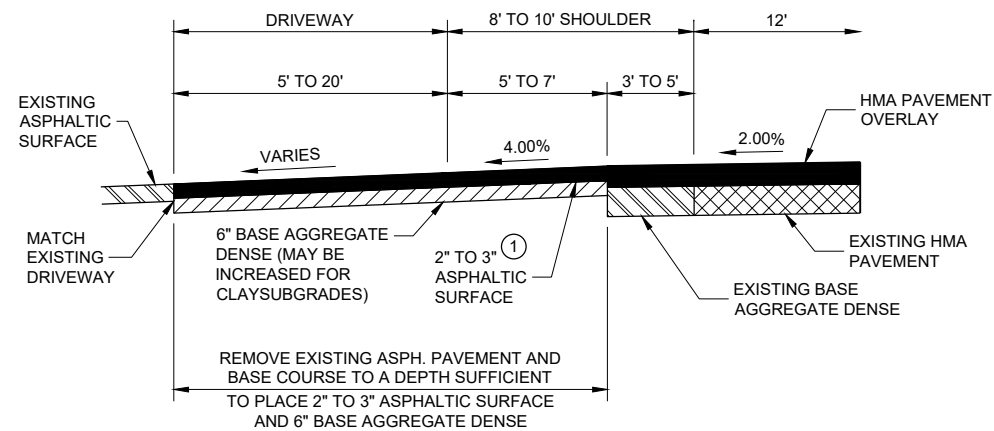


- Ⓐ : PAID FOR AS ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES. (TON)
- Ⓑ : PAID FOR AS BASE AGGREGATE DENSE 1 1/4" (TON)
- ⒲ : DRIVEWAY WIDTH 16' MIN. - 24' MAX.

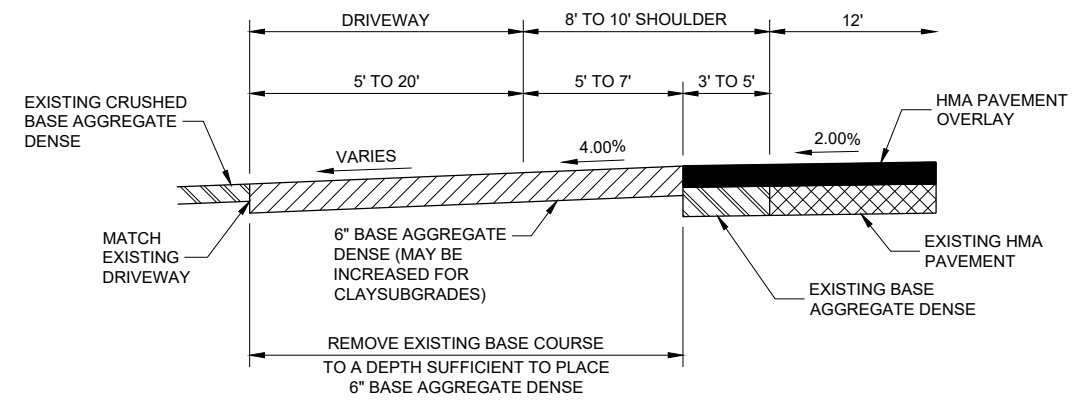
**PLAN VIEW
HALF SECTION**



**PLAN VIEW
HALF SECTION**



**PROFILE VIEW
RURAL ENTRANCE
WITH ASPHALTIC SURFACE
RESURFACING PROJECTS**



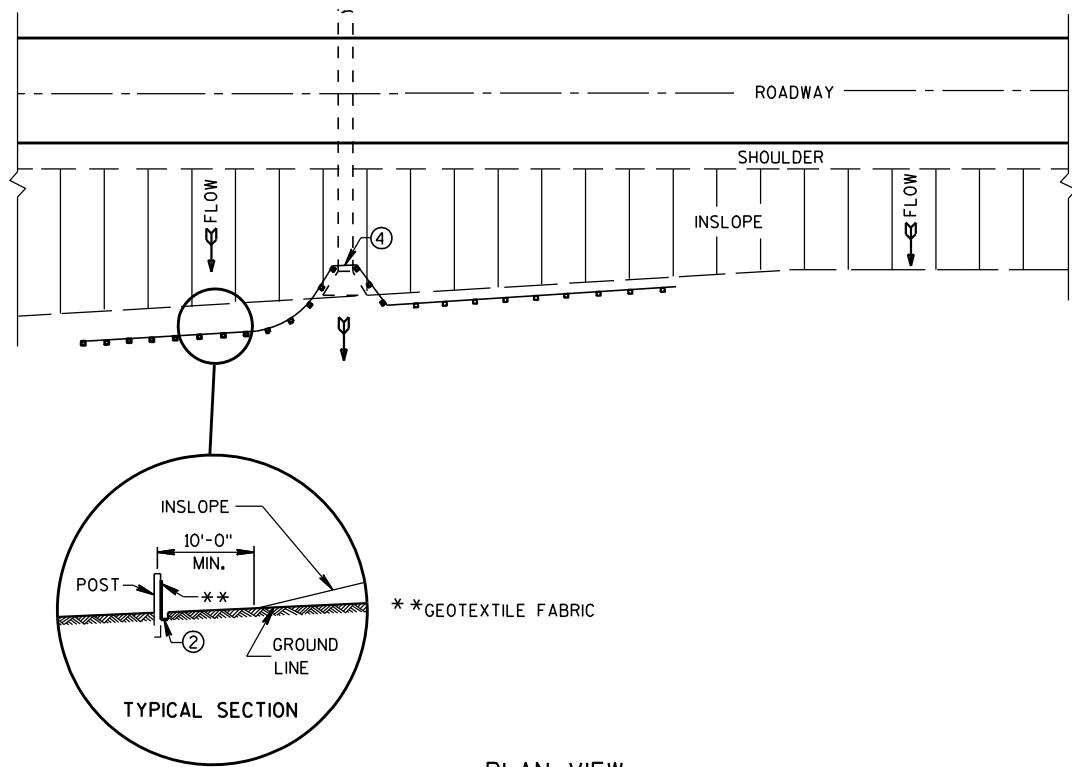
**PROFILE VIEW
RURAL ENTRANCE
WITH AGGREGATE SURFACE
6" BASE AGGREGATE DENSE
RESURFACING PROJECTS**

**DRIVEWAYS WITHOUT CURB
AND GUTTER RESURFACING
PROJECTS RURAL**

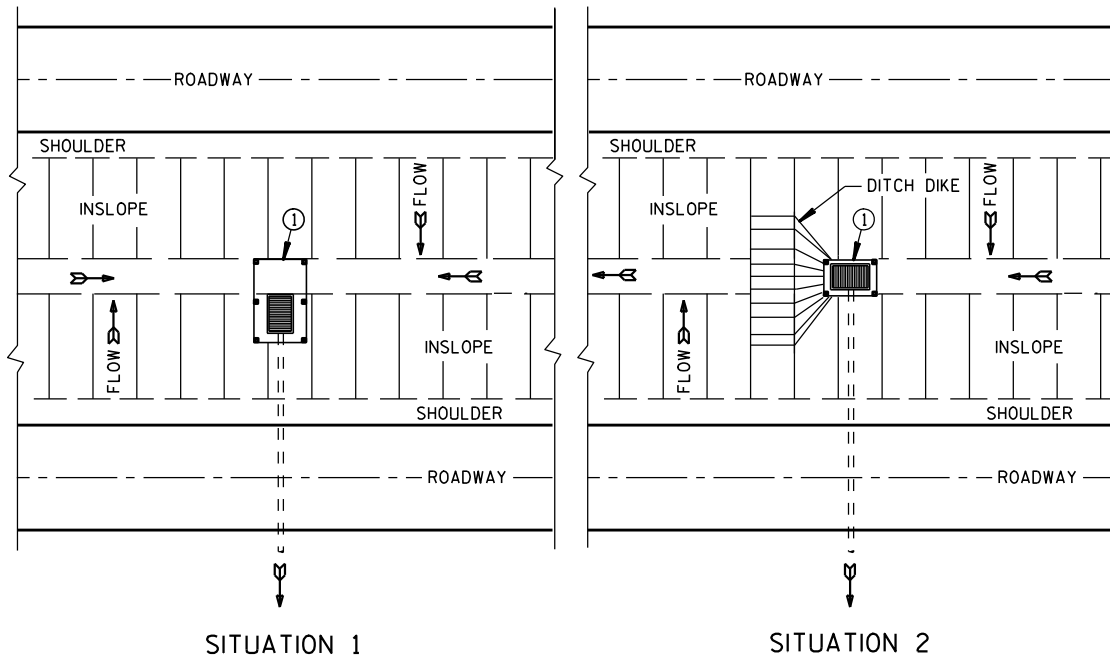
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
December 2016 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
ENGINEER

FHWA



PLAN VIEW
TYPICAL APPLICATION OF SILT FENCE

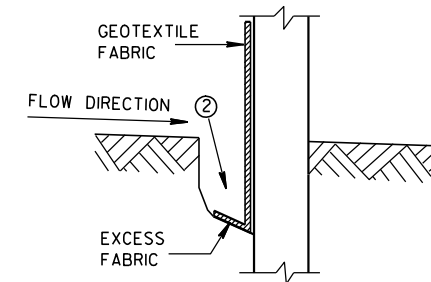


SITUATION 1 SITUATION 2
PLAN VIEW
SILT FENCE AT MEDIAN SURFACE DRAINS

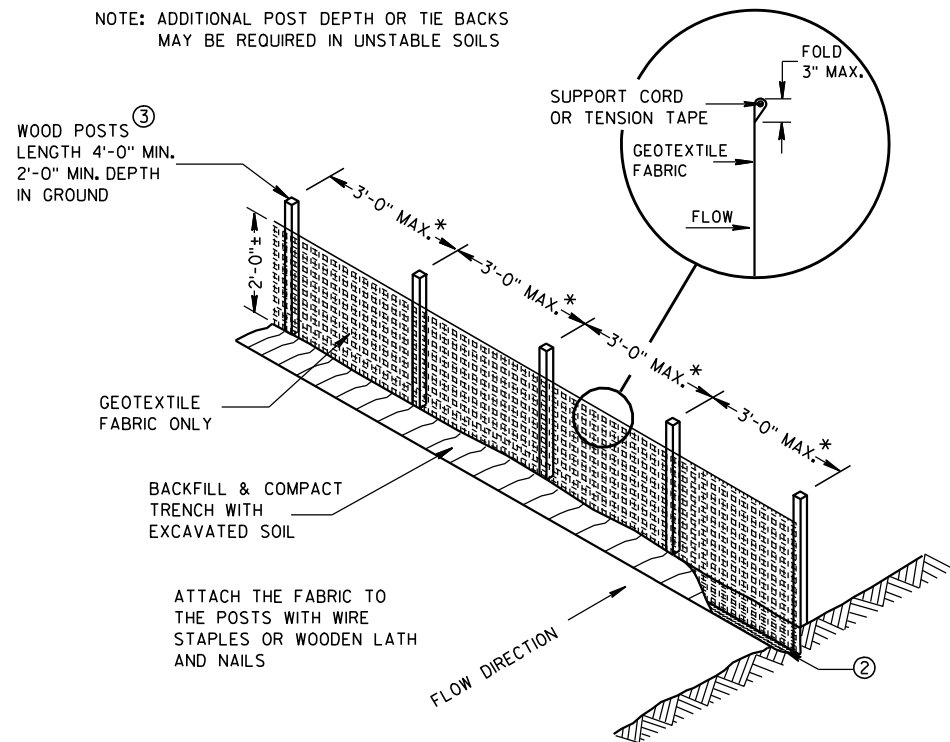
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

- ① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- ③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1 1/8" X 1 1/8" OF OAK OR HICKORY.
- ④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- ⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.

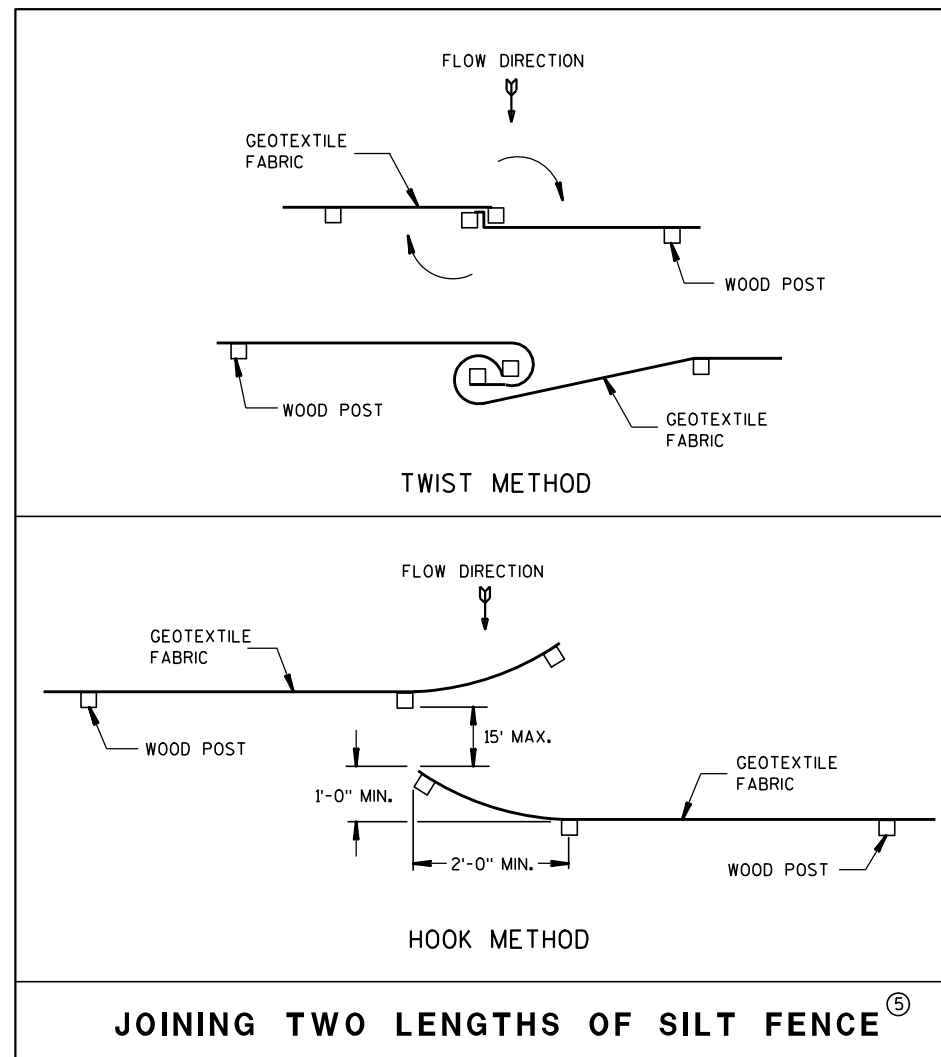


TRENCH DETAIL

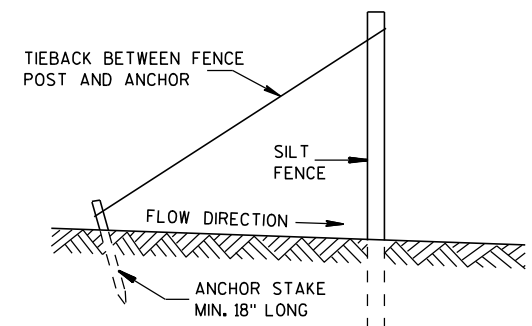


SILT FENCE

* NOTE: 8'-0" POST SPACING ALLOWED IF A WOVEN GEOTEXTILE FABRIC IS USED.

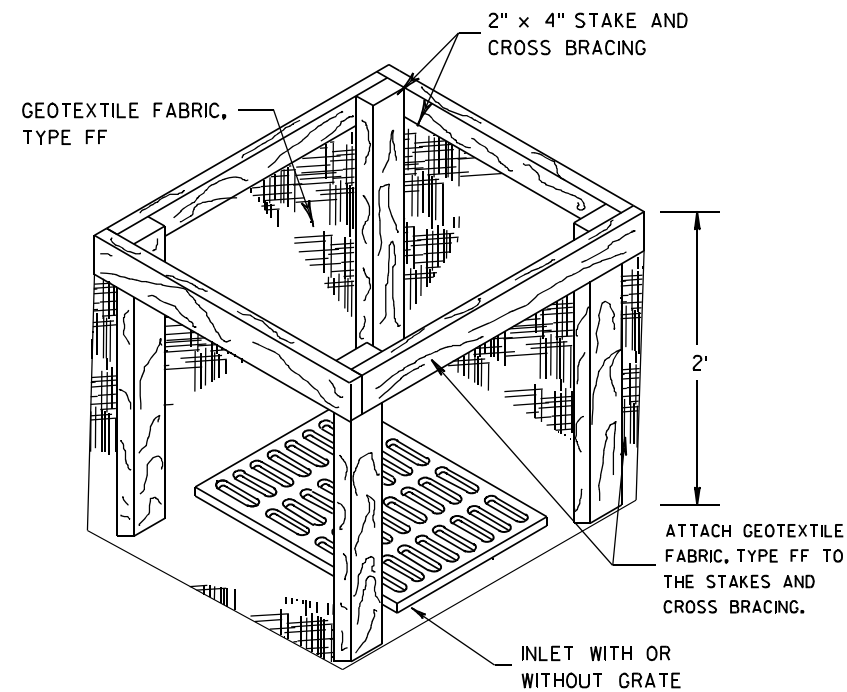
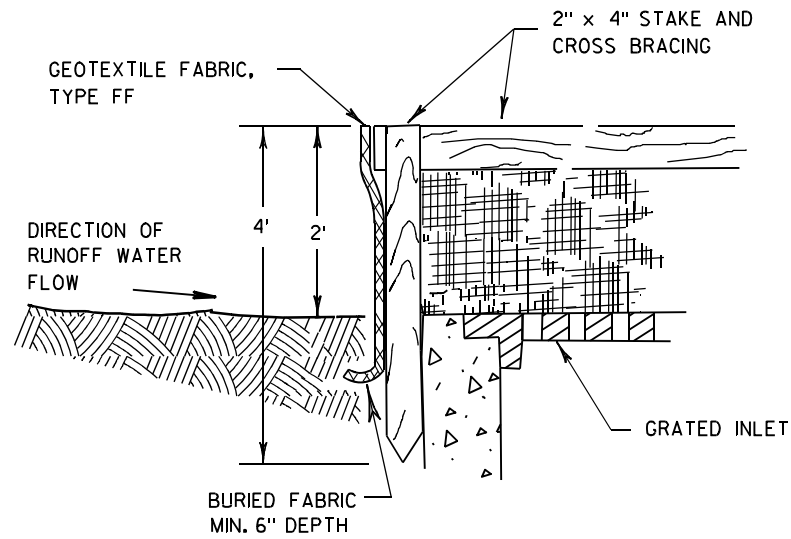


JOINING TWO LENGTHS OF SILT FENCE ⑤



SILT FENCE TIE BACK
(WHEN REQUIRED BY THE ENGINEER)

SILT FENCE	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 4-29-05 DATE	/S/ Beth Canestra CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA	



INLET PROTECTION, TYPE A

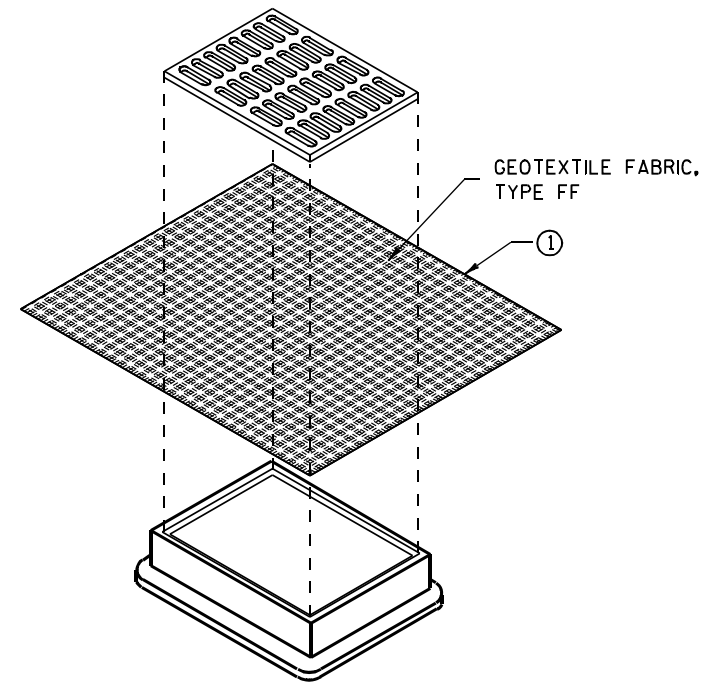
GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

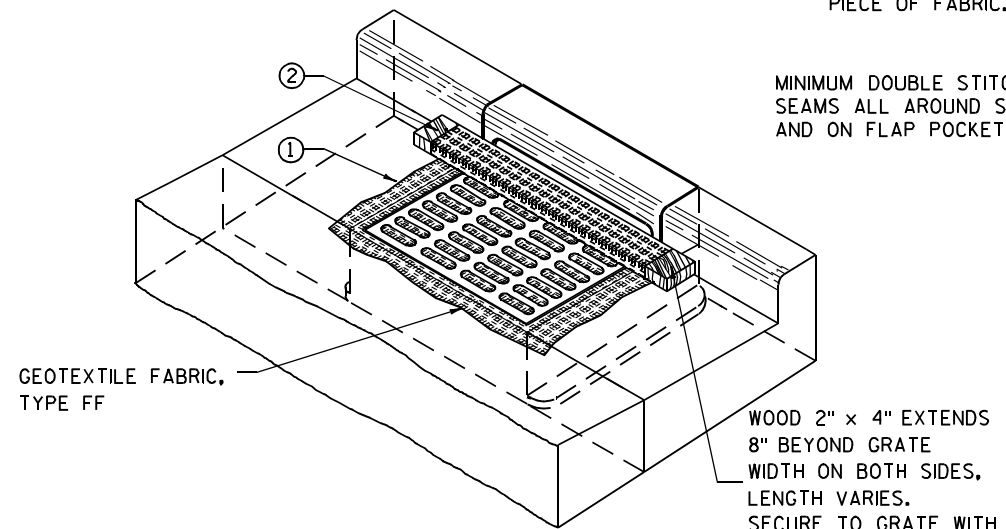
WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- ① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- ② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- ③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



**INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)**

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE C (WITH CURB BOX)

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

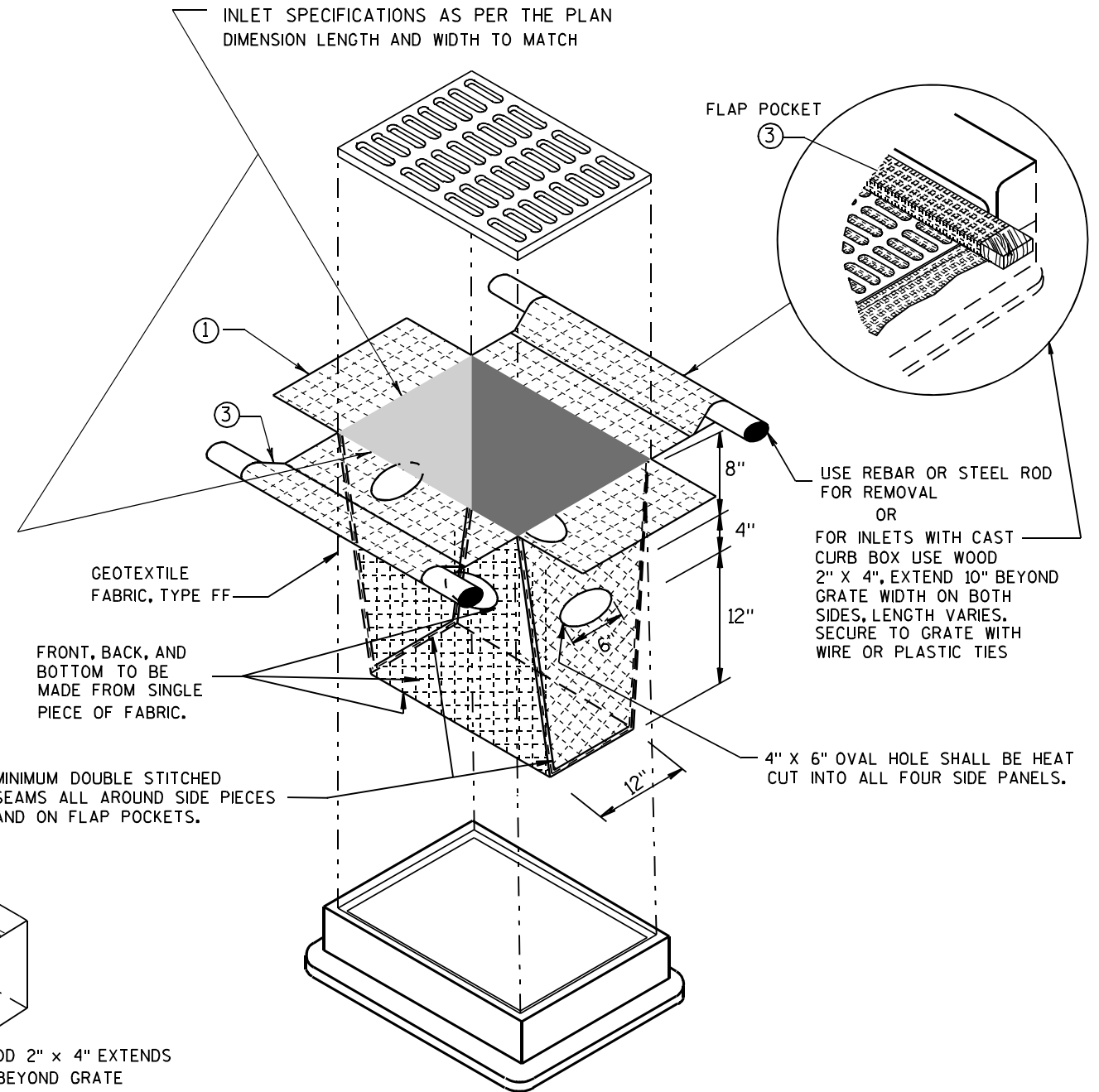
THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

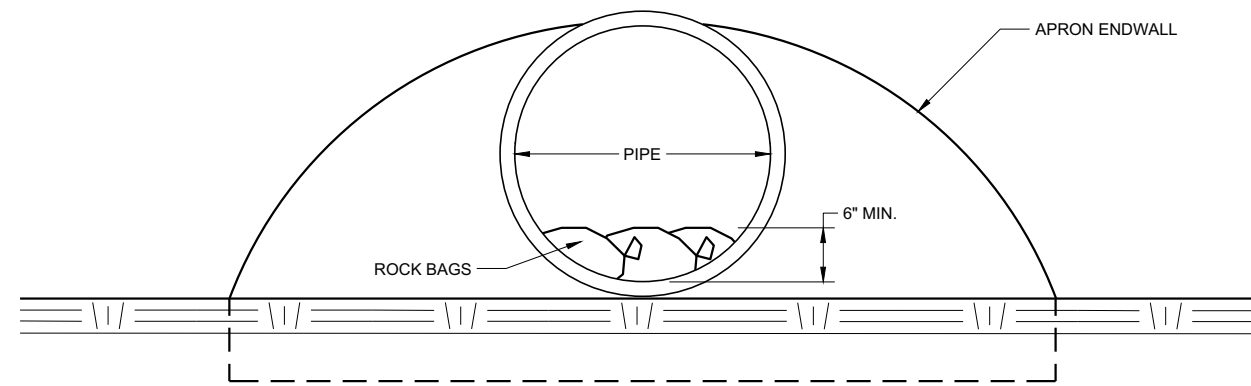
THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.



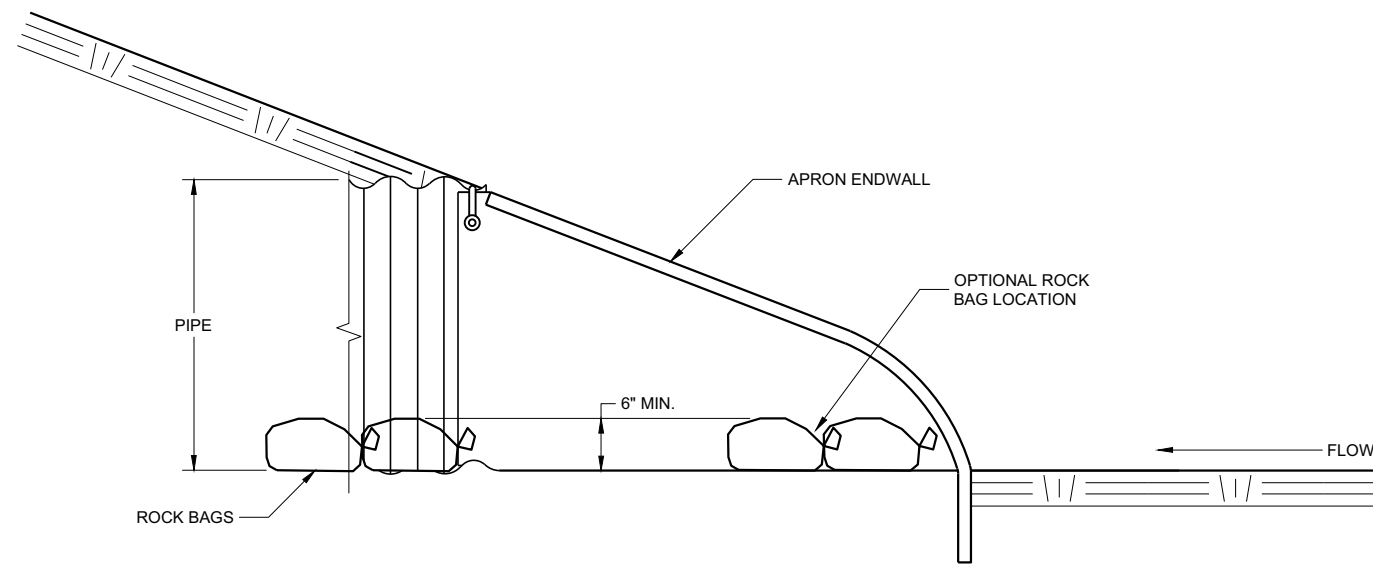
INLET PROTECTION, TYPE D

(CAN BE INSTALLED IN ANY INLET TYPE WITH OR WITHOUT A CURB BOX AS PER NOTE ②)

INLET PROTECTION TYPE A, B, C, AND D	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 10/16/02 DATE	/s/ Beth Connestra CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA	



END VIEW



SIDE VIEW

CULVERT PIPE CHECK
(INSTALL ON INLET END ONLY)

6

6

SDD 08E15 - 01

SDD 08E15 - 01

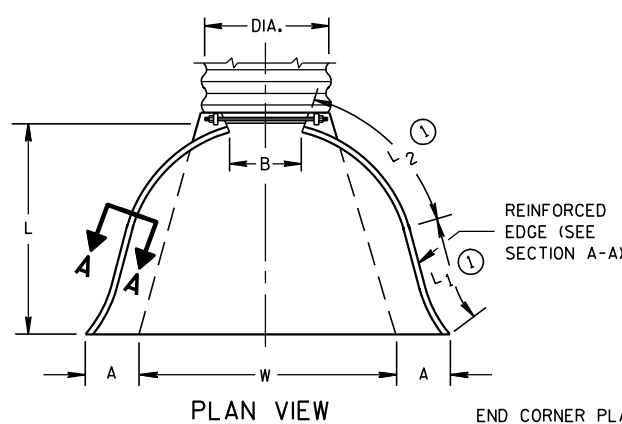
CULVERT PIPE CHECK	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2019 DATE	/S/ Daniel Schave EROSION CONTROL ENGINEER
<small>FHWA</small>	

METAL APRON ENDWALLS											
PIPE DIA. (IN.)	MIN. THICK. (Inches)		DIMENSIONS (Inches)							APPROX. SLOPE	BODY
	STEEL	ALUM.	A (±1")	B (MAX.)	H (±1")	L (±1 1/2")	L1	L2	W (±2")		
12	.064	.060	6	6	6	21	12	17 1/2	24	2 1/2 to 1	1 Pc.
15	.064	.060	7	8	6	26	14	21 3/4	30	2 1/2 to 1	1 Pc.
18	.064	.060	8	10	6	31	15	28 1/4	36	2 1/2 to 1	1 Pc.
21	.064	.060	9	12	6	36	18	29 5/8	42	2 1/2 to 1	1 Pc.
24	.064	.075	10	13	6	41	18	37 1/4	48	2 1/2 to 1	1 Pc.
30	.079	.075	12	16	8	51	18	52 1/4	60	2 1/2 to 1	1 Pc.
36	.079	.105	14	19	9	60	24	59 3/4	72	2 1/2 to 1	2 Pc.
42	.109	.105	16	22	11	69	24	75 5/8	84	2 1/2 to 1	2 Pc.
48	.109	.105	18	27	12	78	24	81	90	2 1/4 to 1	3 Pc.
54	.109	.105	18	30	12	84	30	85 1/2	102	2 1/4 to 1	3 Pc.
60	.109x	.105x	18	33	12	87	—	—	114	2 to 1	3 Pc.
66	.109x	.105x	18	36	12	87	—	—	120	2 to 1	3 Pc.
72	.109x	.105x	18	39	12	87	—	—	126	2 to 1	3 Pc.
78	.109x	.105x	18	42	12	87	—	—	132	1 1/2 to 1	3 Pc.
84	.109x	.105x	18	45	12	87	—	—	138	1 1/2 to 1	3 Pc.
90	.109x	.105x	18	37	12	87	—	—	144	1 1/2 to 1	3 Pc.
96	.109x	.105x	18	35	12	87	—	—	150	1 1/2 to 1	3 Pc.

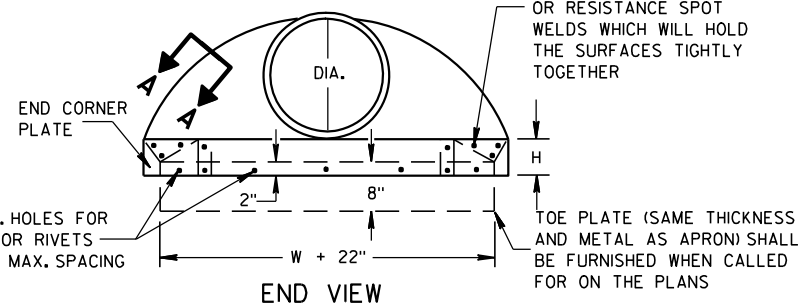
* EXCEPT CENTER PANEL SEE GENERAL NOTES

REINFORCED CONCRETE APRON ENDWALLS									
PIPE DIA. (IN.)	DIMENSIONS (Inches)							APPROX. SLOPE	
	T	A	B	C	D	E	G		
12	2	4	24	48 1/8	72 1/8	24	2	3 to 1	
15	2 1/4	6	27	46	73	30	2 1/4	3 to 1	
18	2 1/2	9	27	46	73	36	2 1/2	3 to 1	
21	2 3/4	9	36	37 1/2	73 1/2	42	2 3/4	3 to 1	
24	3	9 1/2	43 1/2	30	73 1/2	48	3	3 to 1	
27	3 1/4	10 1/2	49 1/2	24	73 1/2	54	3 1/4	3 to 1	
30	3 1/2	12	54	19 3/4	73 1/2	60	3 1/2	3 to 1	
36	4	15	63	34 3/4	97 3/4	72	4	3 to 1	
42	4 1/2	21	63	35	98	78	4 1/2	3 to 1	
48	5	24	72	26	98	84	5	3 to 1	
54	5 1/2	27	65	33 1/4-35	98 1/4-100	90	5 1/2	2 1/2 to 1	
60	6	30-35	60	39	99	96	5	2 to 1	
66	6 1/2	24-30	72-78	21-27	99	102	5 1/2	2 to 1	
72	7	24-36	78	21	99	108	6	2 to 1	
78	7 1/2	24-36	78	21	99	114	6 1/2	2 to 1	
84	8	36	90 1/2	21	111 1/2	120	6 1/2	1 1/2 to 1	
90	8 1/2	41	87 1/2	24	111 1/2	132	6 1/2	1 1/2 to 1	

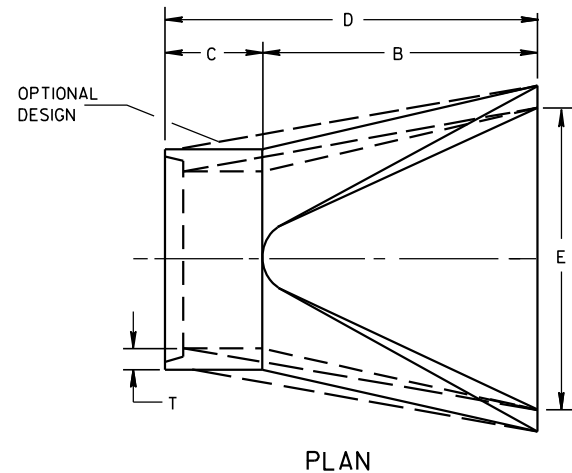
* MINIMUM
** MAXIMUM



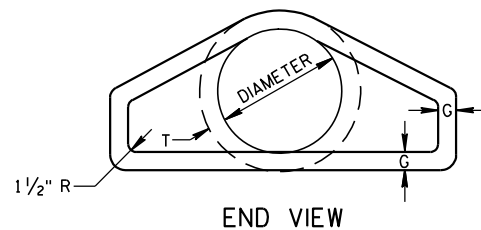
END CORNER PLATES MAY BE FASTENED TO APRON PROPER BY BOLTS, RIVETS, OR RESISTANCE SPOT WELDS WHICH WILL HOLD THE SURFACES TIGHTLY TOGETHER



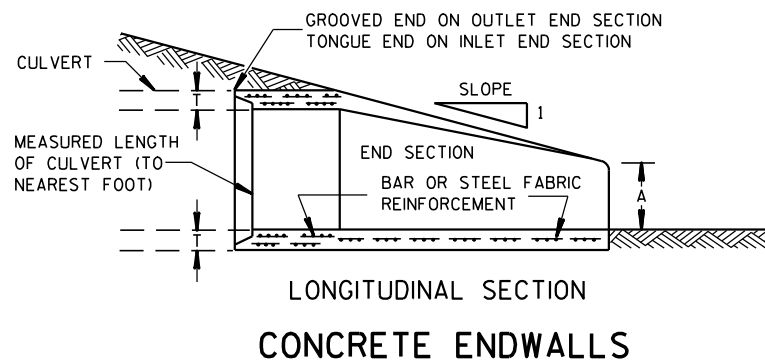
SIDE ELEVATION
METAL ENDWALLS



PLAN

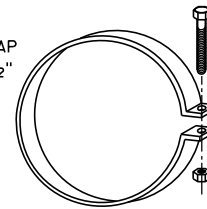


END VIEW

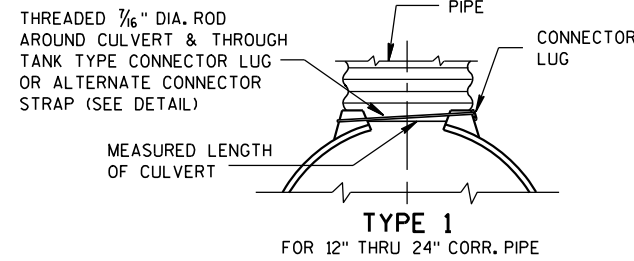


LONGITUDINAL SECTION
CONCRETE ENDWALLS

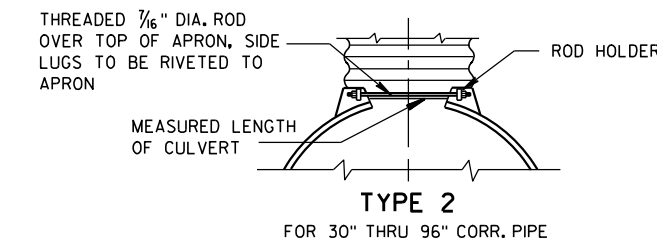
1" WIDE, 12 GA. (0.109" THICK) GALVANIZED STRAP WITH STANDARD 6" X 1/2" BAND BOLT AND NUT



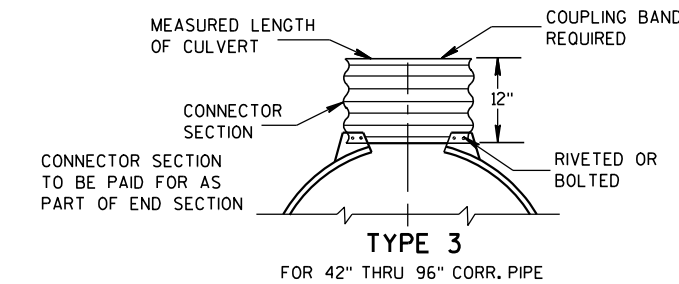
ALTERNATE FOR TYPE 1 CONNECTION
END SECTION CONNECTOR STRAP



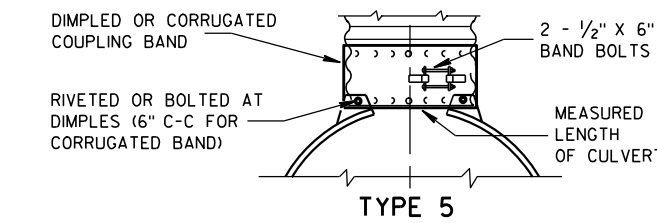
TYPE 1
FOR 12" THRU 24" CORR. PIPE



TYPE 2
FOR 30" THRU 96" CORR. PIPE



TYPE 3
FOR 42" THRU 96" CORR. PIPE



ALTERNATE FOR:
ALL SIZES CORRUGATED CIRCULAR PIPE

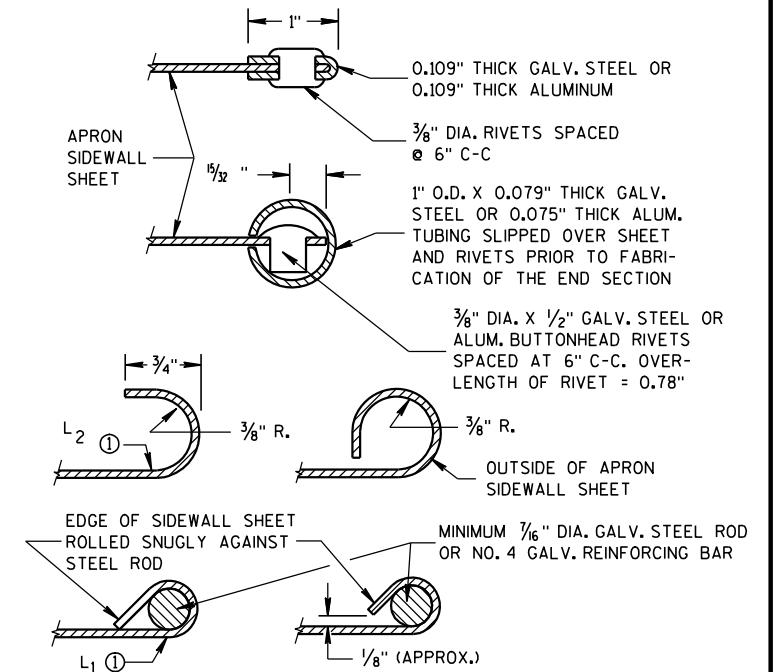
NOTE: DIMPLED BAND FITS OVER OUTSIDE OF ENDWALL, AND CORRUGATED BAND FITS INSIDE ENDWALL. DIMPLED BAND MAY BE USED WITH HELICALLY CORRUGATED PIPE.

FOR CIRCUMFERENTIALLY CORRUGATED PIPE USE ENDWALL CONNECTION DETAILS 1, 2, 3 OR 5 AS APPLICABLE.

FOR HELICALLY CORRUGATED PIPE USE ENDWALL CONNECTION DETAILS 1, 2 OR 5.

FOR HELICALLY CORRUGATED PIPES WITH TWO CIRCUMFERENTIAL CORRUGATIONS AT EACH END USE ENDWALL CONNECTION DETAILS 1, 2 OR 3.

CONNECTION DETAILS



SECTION A-A

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

CONCRETE CULVERT ENDWALLS MAY NOT BE USED WITH GALVANIZED STEEL OR ALUMINUM CULVERT PIPE OR VICE VERSA. GALVANIZED STEEL OR ALUMINUM ENDWALLS SHALL NORMALLY BE INSTALLED ON CULVERT PIPE OF THE SAME METAL.

ALL THREE PIECE STEEL APRON ENDWALLS FOR 60" DIAMETER PIPE AND LARGER SHALL HAVE 0.109" SIDES AND 0.138" CENTER PANELS. ALL THREE PIECE ALUMINUM APRON ENDWALLS FOR 60" DIAMETER PIPE AND LARGER SHALL HAVE 0.105" SIDES AND 0.134" CENTER PANELS. THE WIDTH OF CENTER PANELS SHALL BE GREATER THAN 20 PERCENT OF THE PIPE PERIMETER.

LAP SEAMS SHALL BE TIGHTLY JOINED BY GALVANIZED RIVETS OR BOLTS FOR STEEL UNITS AND ALUMINUM RIVETS AND BOLTS FOR ALUMINUM UNITS. FOR THE 60" THROUGH 96" DIAMETER APRON ENDWALL SIZES, THE REINFORCED EDGES AND CENTER PANEL SEAMS SHALL BE FURTHER REINFORCED WITH GALVANIZED STEEL OR ALUMINUM STIFFENER ANGLES. THE ANGLES SHALL BE ATTACHED BY GALVANIZED NUTS AND BOLTS FOR STEEL UNITS AND ALUMINUM NUTS AND BOLTS FOR ALUMINUM UNITS.

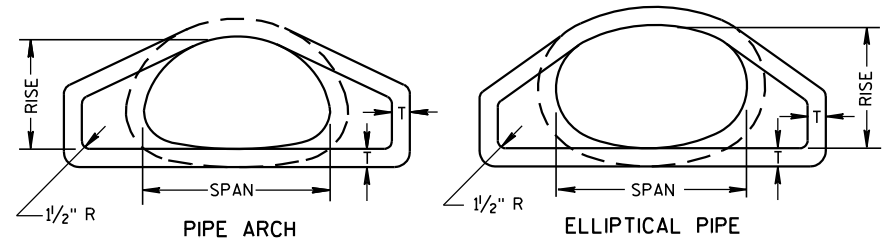
WHERE TWO OR MORE PIPES WITH APRON ENDWALLS ARE LAID ADJACENT TO EACH OTHER, THEY SHALL BE SEPARATED BY A DISTANCE SUFFICIENT TO PROVIDE A MINIMUM CLEARANCE OF 6 INCHES BETWEEN APRON ENDWALLS.

① FOR PIPE SIZES UP TO 60" DIAMETER, A 180° ROLLED EDGE MAY BE USED INSTEAD OF STEEL ROD REINFORCEMENT. SEE SECTION A-A.

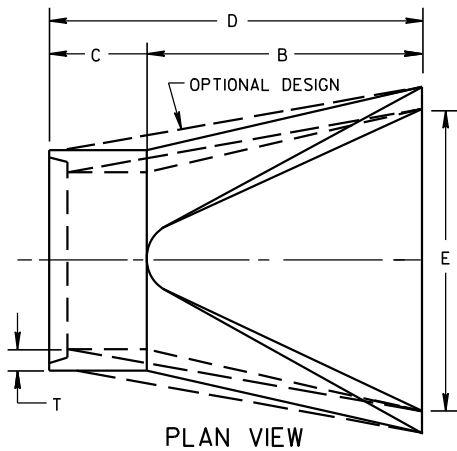
APRON ENDWALLS FOR
CULVERT PIPE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

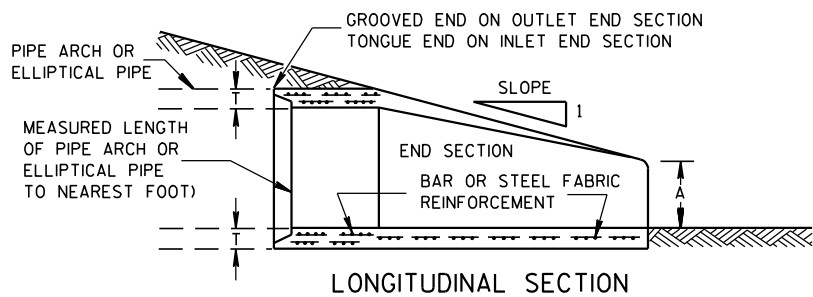
APPROVED
11/30/94 /S/ Rory L. Rhinesmith
DATE CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA



END VIEW



PLAN VIEW



LONGITUDINAL SECTION

CONCRETE ENDWALLS

2- 2 1/3" X 1/2" CORRUGATIONS

EQUIV. DIA. (Inches)	(Inches)		MIN. THICK. (Inches)		DIMENSIONS (Inches)							APPROX. SLOPE	BODY
	SPAN	RISE	STEEL	ALUM.	A	B	H	L	L1	L2	W		
					(±1")	(MAX.)	(±1")	(±1 1/2")	⊙	⊙	(±2")		
15	17	13	.064	.060	7	9	6	19	14	16	30	2 1/2 to 1	1 Pc.
18	21	15	.064	.060	7	10	6	23	14	19 3/8	36	2 1/2 to 1	1 Pc.
21	24	18	.064	.060	8	12	6	28	18	21 3/4	42	2 1/2 to 1	1 Pc.
24	28	20	.064	.060	9	14	6	32	18	27 1/2	48	2 1/2 to 1	1 Pc.
30	35	24	.079	.075	10	16	6	39	18	37 5/8	60	2 1/2 to 1	1 Pc.
36	42	29	.079	.075	12	18	8	46	24	45 3/8	75	2 1/2 to 1	1 Pc.
42	49	33	.109	.105	13	21	9	53	24	54 3/4	85	2 1/2 to 1	2 Pc.
48	57	38	.109	.105	18	26	12	63	24	68	90	2 1/2 to 1	3 Pc.
54	64	43	.109	.105	18	30	12	70	24	72 3/4	102	2 1/4 to 1	3 Pc.
60	71	47	.109*	.105*	18	33	12	77	30	82 1/4	114	2 1/4 to 1	3 Pc.
66	77	52	.109*	.105*	18	36	12	77	—	—	126	2 to 1	3 Pc.
72	83	57	.109*	.105*	18	39	12	77	—	—	138	2 to 1	3 Pc.

3" X 1" CORRUGATIONS

EQUIV. DIA. (Inches)	(Inches)		MIN. THICK. (Inches)		DIMENSIONS (Inches)							APPROX. SLOPE	BODY
	SPAN	RISE	STEEL	ALUM.	A	B	H	L	L1	L2	W		
					(±1")	(MAX.)	(±1")	(±1 1/2")	⊙	⊙	(±2")		
48	53	41	.109	.105	18	26	12	63	24	72 3/4	90	2 1/2 to 1	2 Pc.
54	60	46	.109	.105	18	30	12	70	30	82 1/4	102	2 to 1	2 Pc.
60	66	51	.109*	.105*	18	33	12	77	—	—	114	1 1/2 to 1	3 Pc.
66	73	55	.109*	.105*	18	36	12	77	—	—	126	1 1/2 to 1	3 Pc.
72	81	59	.109*	.105*	18	39	12	77	—	—	138	2 to 1	3 Pc.
78	87	63	.109*	.105*	22	38	12	77	—	—	148	1 1/2 to 1	3 Pc.
84	95	67	.109*	.105*	22	34	12	77	—	—	162	1 1/2 to 1	3 Pc.
90	103	71	.109*	.105*	22	38	12	77	—	—	174	1 1/2 to 1	3 Pc.
96	112	75	.109*	.105*	24	40	12	77	—	—	174	1 1/2 to 1	3 Pc.

NOTE: ALL SPLICES TO BE LAP RIVETED OR BOLTED. * EXCEPT CENTER PANEL SEE GENERAL NOTES

REINFORCED CONCRETE PIPE ARCH

EQUIV. DIA. (Inches)	DIMENSIONS (Inches)								APPROX. SLOPE
	** SPAN	** RISE	T	A	B	C	D	E	
24	29	18	3	8 1/2	39	33	72	48	3 to 1
30	36	22	3 1/2	9 1/2	50	46	96	60	3 to 1
36	44	27	4	11 1/8	60	36	96	72	3 to 1
42	51	31	4 1/2	15 5/16	60	36	96	78	3 to 1
48	58	36	5	21	60	36	96	84	3 to 1
54	65	40	5 1/2	25 1/2	60	36	96	90	3 to 1
60	73	45	6	31	60	36	96	96	3 to 1
72	88	54	7	31	60	39	99	120	2 to 1
84	102	62	8	28 1/2	83	19	102	144	2 to 1

REINFORCED CONCRETE ELLIPTICAL PIPE

EQUIV. DIA. (Inches)	DIMENSIONS (Inches)								APPROX. SLOPE
	** SPAN	** RISE	T	A	B	C	D	E	
24	30	19	3 1/4	8 1/2	39	33	72	48	3 to 1
30	38	24	3 3/4	9 1/2	54	18	72	60	3 to 1
36	45	29	4 1/2	11 1/8	60	24	84	72	2 1/2 to 1
42	53	34	5	15 3/4	60	36	96	78	2 1/2 to 1
48	60	38	5 1/2	21	60	36	96	84	2 1/2 to 1
54	68	43	6	25 1/2	60	36	96	90	2 1/2 to 1
60	76	48	6 1/2	30	60	36	96	96	2 1/2 to 1

** NOMINAL SIZE

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

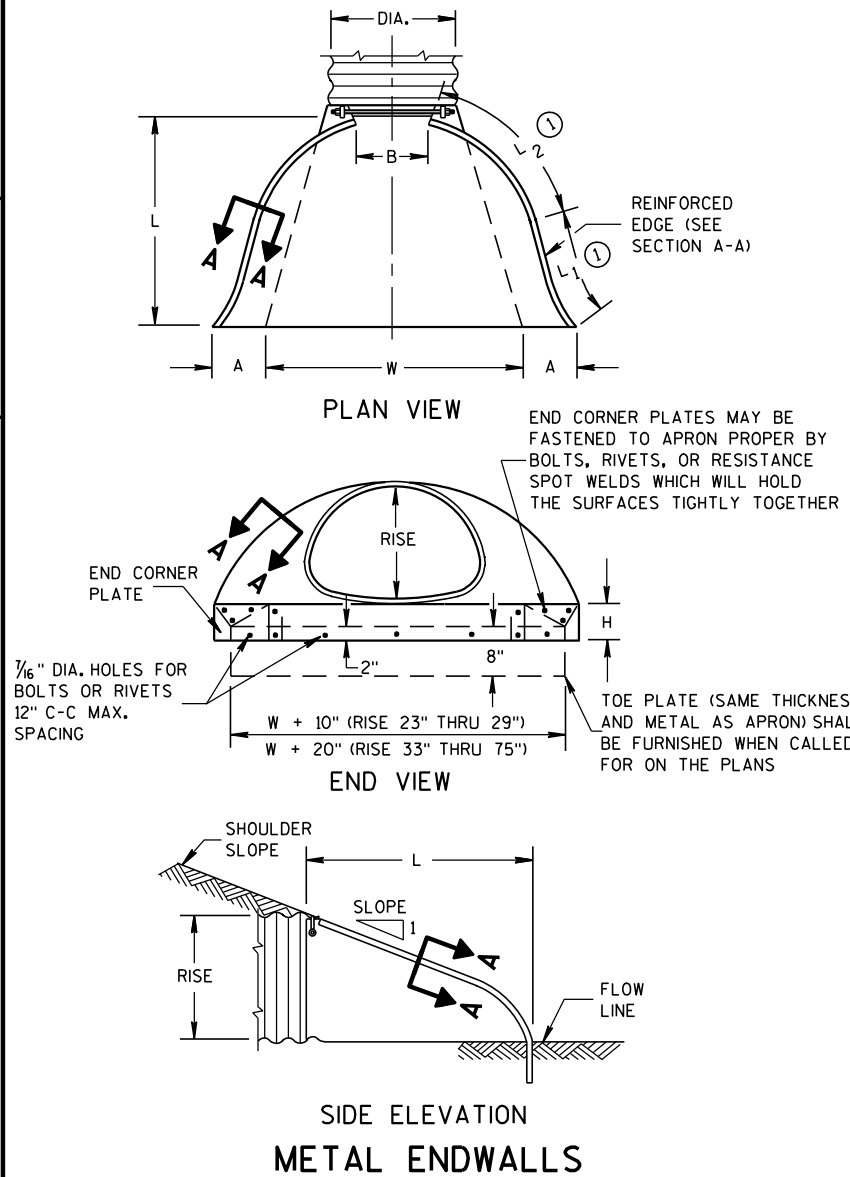
CONCRETE APRON ENDWALLS MAY NOT BE USED WITH GALVANIZED STEEL OR ALUMINUM CULVERT PIPE OR VISE VERSA. GALVANIZED STEEL OR ALUMINUM APRON ENDWALLS SHALL NORMALLY BE INSTALLED ON CULVERT PIPE OF THE SAME METAL.

ALL THREE PIECE STEEL APRON ENDWALLS FOR 66" X 51" PIPE ARCH AND LARGER SHALL HAVE 0.109" SIDES AND 0.138" CENTER PANELS. ALL THREE PIECE ALUMINUM APRON ENDWALLS FOR 66" X 51" PIPE ARCH AND LARGER SHALL HAVE 0.105" SIDES AND 0.134" CENTER PANELS. THE WIDTH OF CENTER PANELS SHALL BE GREATER THAN 20 PERCENT OF THE PIPE ARCH PERIMETER.

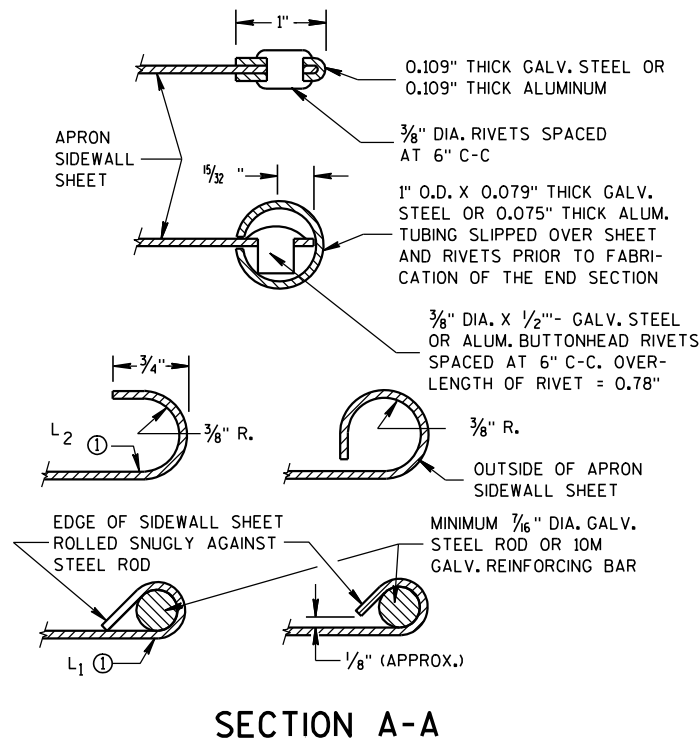
LAP SEAMS SHALL BE TIGHTLY JOINED BY GALVANIZED RIVETS OR BOLTS FOR STEEL UNITS AND ALUMINUM RIVETS AND BOLTS FOR ALUMINUM UNITS. FOR THE 77" X 52" THROUGH 112" X 75" APRON ENDWALL SIZES, THE REINFORCED EDGES AND CENTER PANEL SEAMS SHALL BE FURTHER REINFORCED WITH GALVANIZED STEEL OR ALUMINUM STIFFENER ANGLES. THE ANGLES SHALL BE ATTACHED BY GALVANIZED NUTS AND BOLTS FOR STEEL UNITS AND ALUMINUM NUTS AND BOLTS FOR ALUMINUM UNITS.

WHERE TWO OR MORE PIPES WITH APRON ENDWALLS ARE LAID ADJACENT TO EACH OTHER, THEY SHALL BE SEPARATED BY A DISTANCE SUFFICIENT TO PROVIDE A MINIMUM CLEARANCE OF 6 INCHES BETWEEN APRON ENDWALLS.

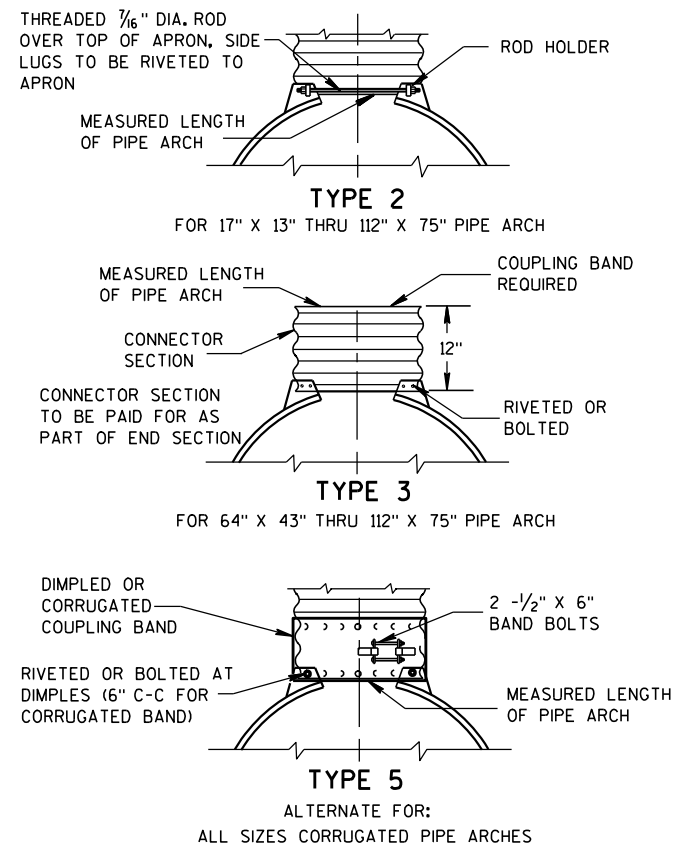
① FOR PIPE ARCH SIZES UP TO 73" X 55" A 180° ROLLED EDGE MAY BE USED INSTEAD OF STEEL ROD REINFORCEMENT. SEE SECTION A-A.



SIDE ELEVATION METAL ENDWALLS



SECTION A-A



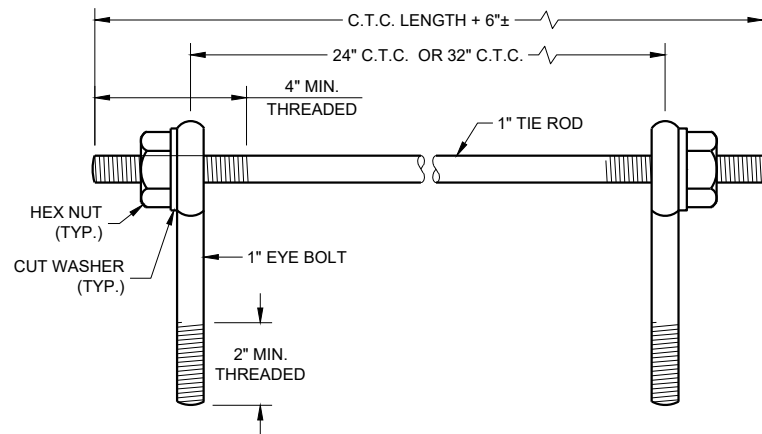
CONNECTION DETAILS

NOTE: DIMPLER BAND FITS OVER OUTSIDE OF ENDWALL, AND CORRUGATED BAND FITS INSIDE ENDWALL.

APRON ENDWALLS FOR PIPE ARCH AND ELLIPTICAL PIPE

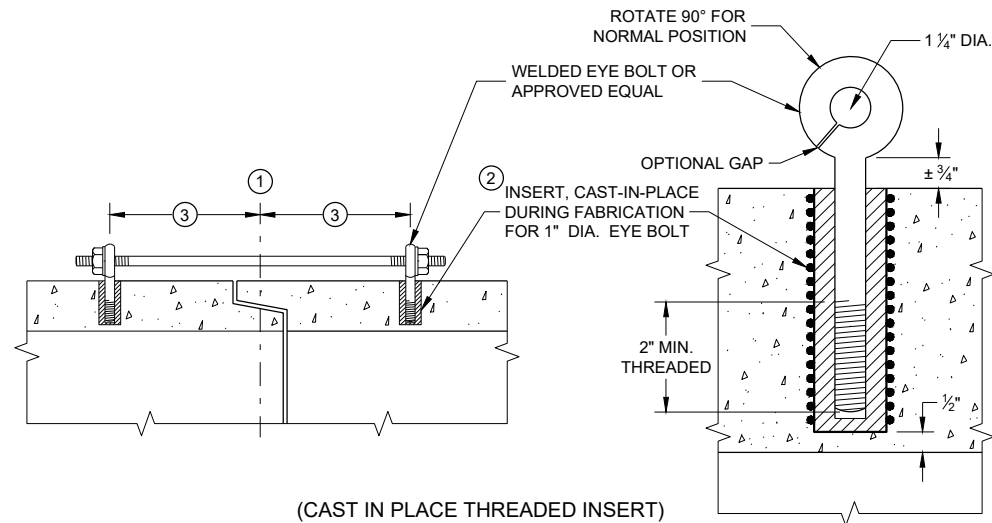
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
11/30/94 DATE /S/ Rory L. Rhinesmith
CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA



EYE BOLTS AND TIE ROD

EYE BOLT AND TIE ROD ASSEMBLY (ALTERNATE NO. 1)



LONGITUDINAL SECTIONS

GENERAL NOTES

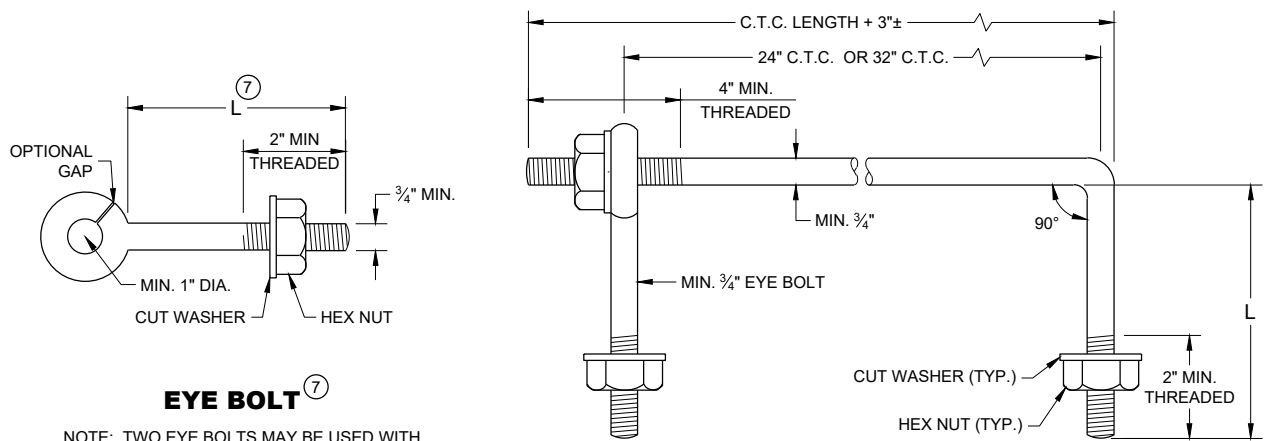
DETAILS OF CONSTRUCTION, MATERIALS, AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

CONCRETE CULVERT AND STORM SEWER PIPE SHALL BE TIED TOGETHER IN THE MANNER ILLUSTRATED BY THIS DETAIL AT LOCATIONS DESIGNATED IN THE STANDARD SPECIFICATIONS AND THE PLAN. THE CONTRACTOR MAY USE EITHER ALTERNATE 1, 2 OR 3 FOR DRAINAGE STRUCTURES. ONLY ALTERNATE 1 AND 3 MAY BE USED FOR CATTLE PASSES, UNLESS OTHERWISE STATED IN THE CONTRACT. THE MATERIALS, FABRICATION AND WORK NECESSARY TO TIE THE PIPE BY THIS DETAIL WILL BE CONSIDERED INCIDENTAL TO THE PIPE AND APRON ENDWALLS IF REQUIRED.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR JOINT TIES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL.

JOINT TIES TO BE HOT-DIP GALVANIZED PER ASTM A 153.

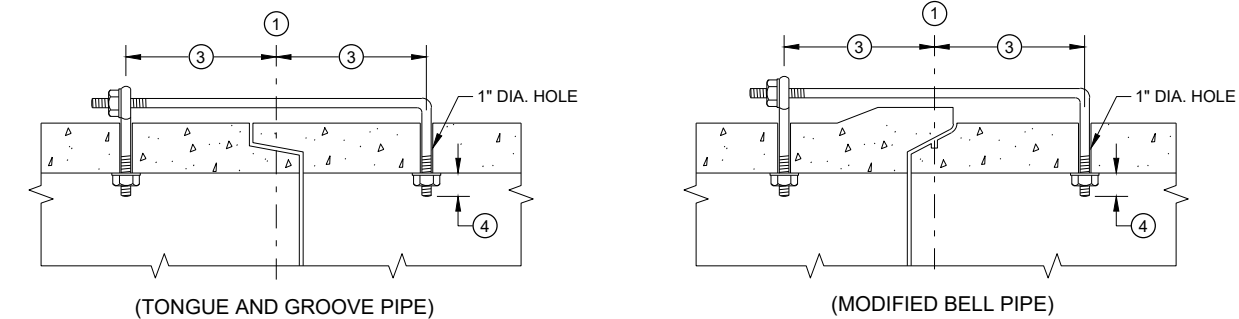
- ① CENTER LINE OF TONGUE AND GROOVE OR BELL AND SPIGOT JOINTS.
- ② THE INSIDE OF THE THREADED INSERTS SHALL BE CLEAN TO ALLOW THE INSERTION OF THREADED EYE BOLTS.
- ③ HOLES SHALL BE CAST-IN-PLACE OR DRILLED PER THE APPLICABLE DETAIL, AND EQUAL DISTANCE FROM THE CENTERLINE OF THE JOINT.
- ④ BOLT PROJECTION INSIDE OF PIPE SHALL NOT EXCEED 2 INCHES.
- ⑤ OPENING TO BE ROD DIAMETER PLUS 1 INCH.
- ⑥ LENGTH ADEQUATE TO EXTEND TO WITHIN 1/2 INCH OF THE INNER SURFACE OF THE PIPE.
- ⑦ EYE BOLT LENGTH DETERMINED BY WALL THICKNESS, BELL THICKNESS AND BOLT PROJECTION INSIDE PIPE.



EYE BOLT AND TIE ROD

EYE BOLT ⑦

NOTE: TWO EYE BOLTS MAY BE USED WITH A 30" OR 38" LONG THREADED ROD IN LIEU OF THE 90° BENT TIE ROD.



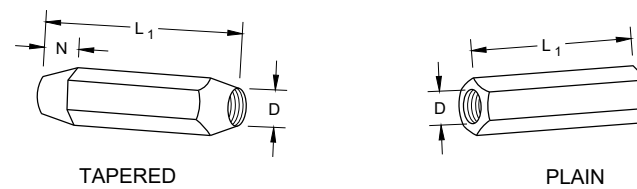
LONGITUDINAL SECTION
(JOINT TIES FOR 18" TO 66" DIA. CONCRETE PIPE)

EYE BOLT AND TIE ROD ASSEMBLY (ALTERNATE NO. 2)

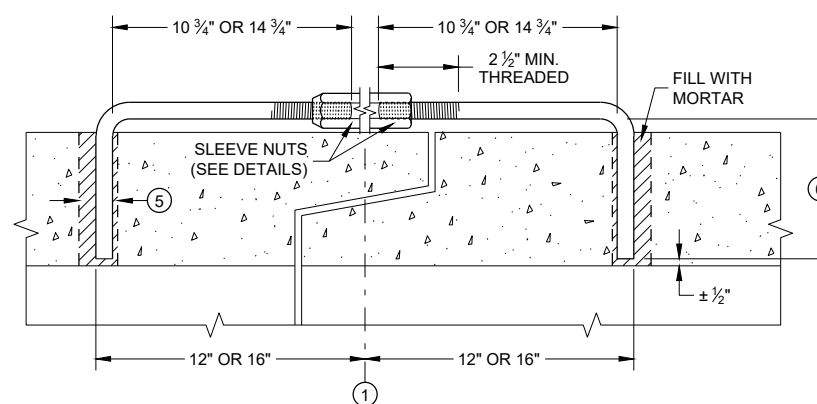
ADJUSTABLE TIE ROD TABLE

PIPE DIAMETER	TIE ROD DIAMETER	D	L ₁	N
12 - 60	5/8	5/8	5	1/2
66 - 84	3/4	3/4	5	1/2
90 - 144	1	1	7	1 7/16

DIMENSIONS SHOWN ARE IN INCHES

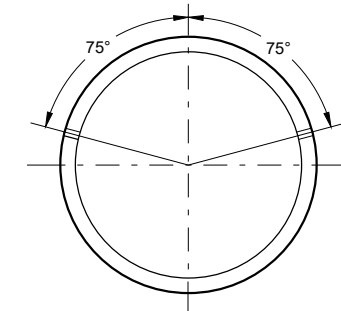


RIGHT AND LEFT THREADS SLEEVE NUTS



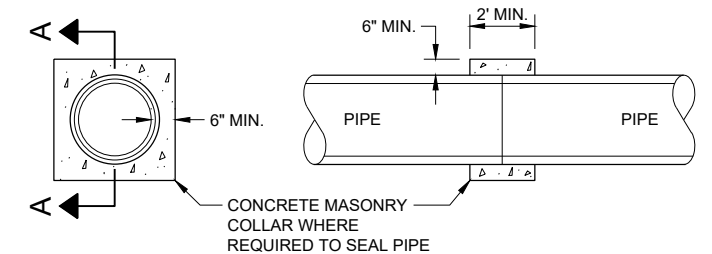
LONGITUDINAL SECTION

ADJUSTABLE TIE ROD (ALTERNATE NO. 3)



PLACEMENT OF (2) CAST-IN-PLACE INSERTS OR HOLES DURING FABRICATION FOR PIPE SECTIONS REQUIRING TIE RODS

TRANSVERSE SECTION



SECTION A - A
CONCRETE COLLAR DETAIL

JOINT TIES FOR CONCRETE PIPE AND CONCRETE COLLAR DETAIL

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

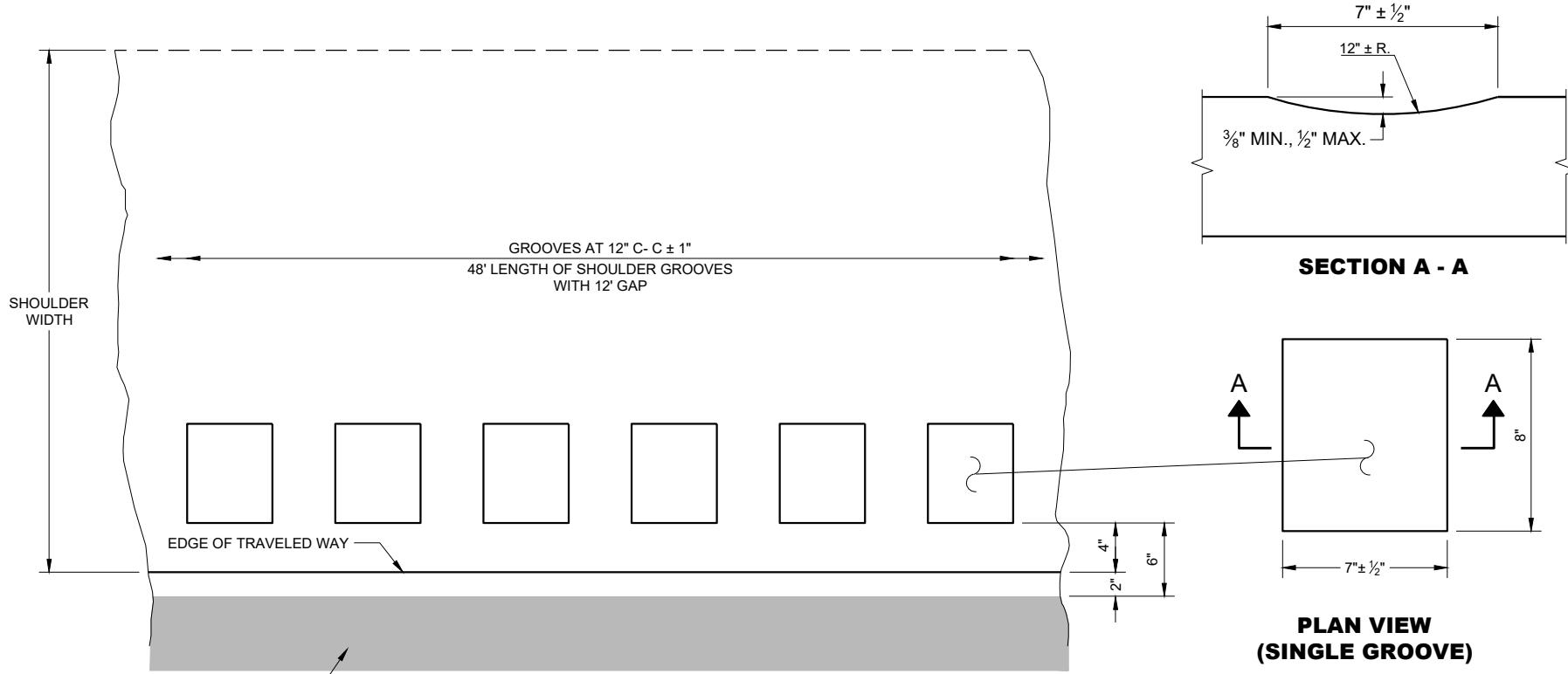
APPROVED
November 2021 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT ENGINEER

FHWA

GENERAL NOTES

DO NOT MILL SHOULDER GROOVES THROUGH INTERSECTIONS, MARKED CROSSWALKS, NON-MOTORIZED PATH CROSSINGS, ETC. REFER TO SDD 13A10 SHEETS "g" AND "h".

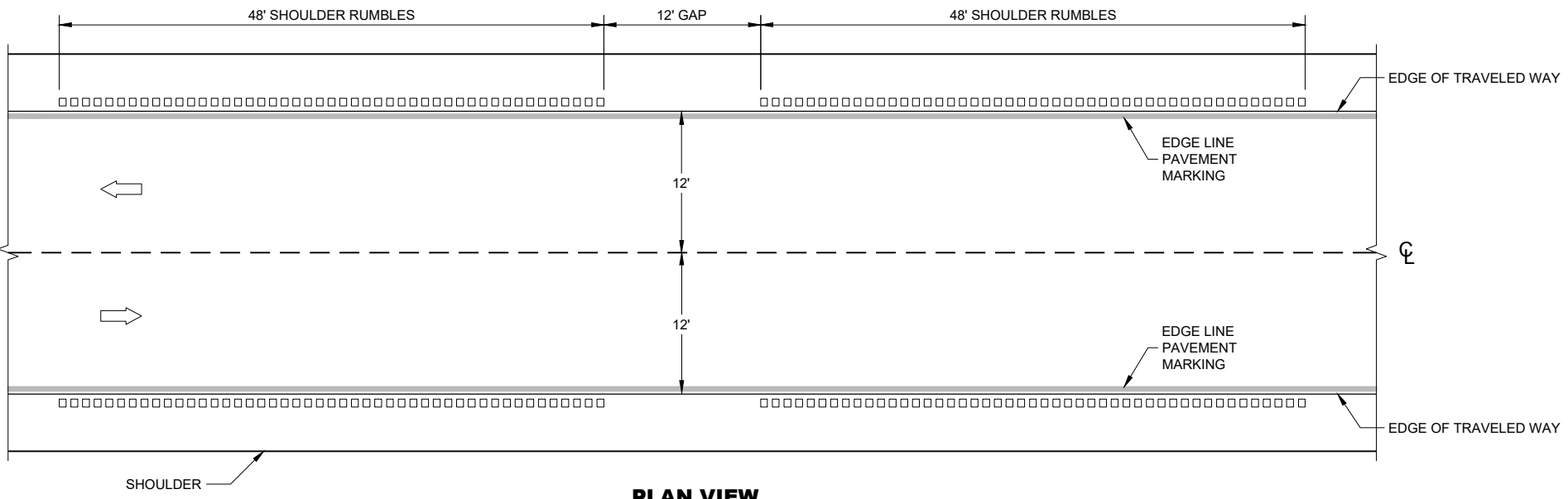
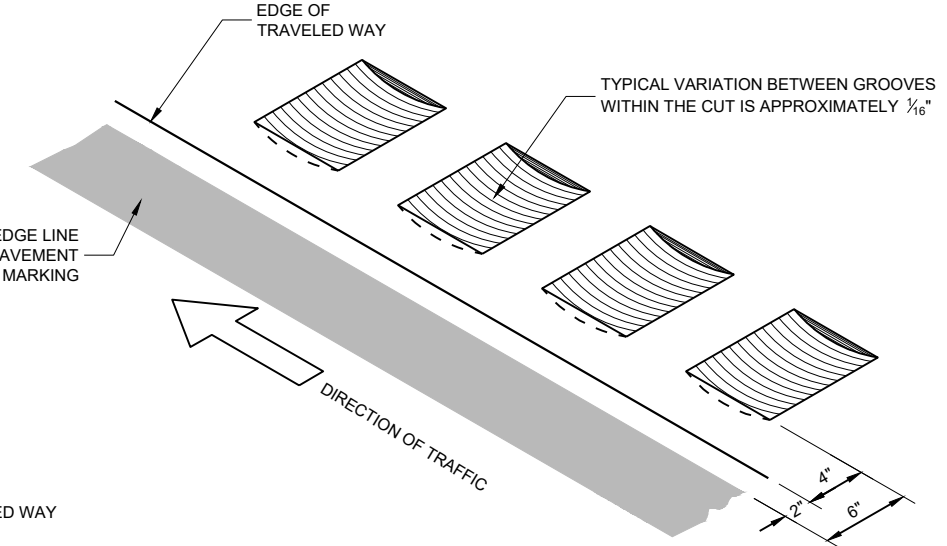
SHOULDER GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS WHEN DIRECTED BY THE ENGINEER.



PLAN DETAIL VIEW SHOULDER WITH GROOVES

SECTION A - A

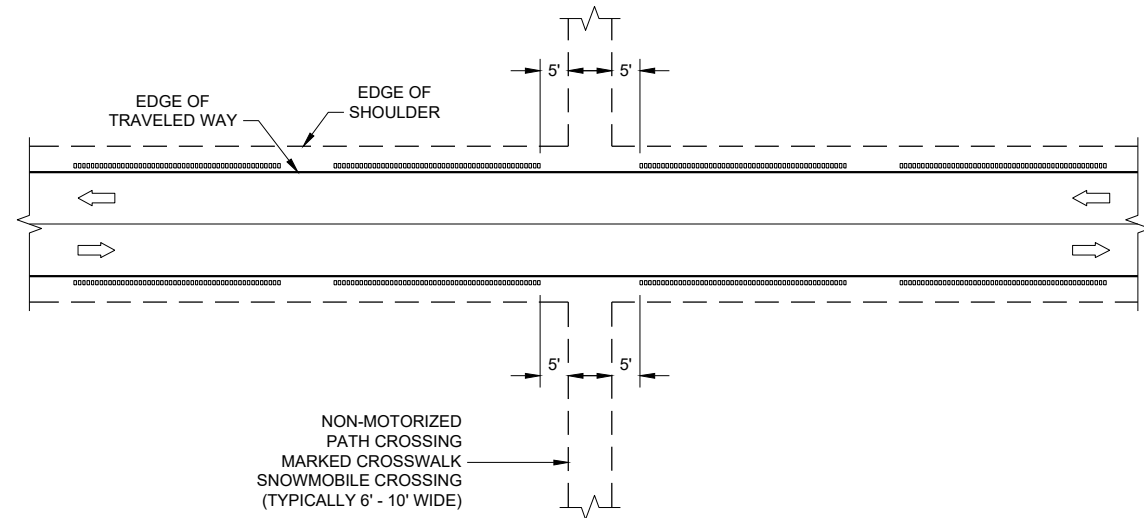
PLAN VIEW (SINGLE GROOVE)



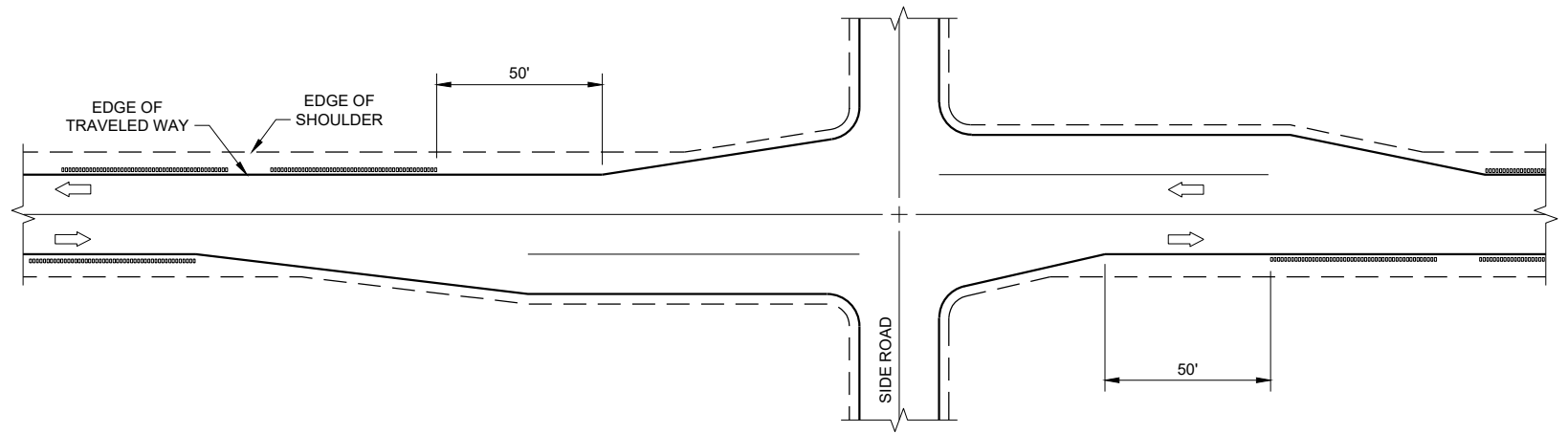
SHOULDER RUMBLE STRIPS - ASPHALT

SHOULDER RUMBLE STRIPS ASPHALT

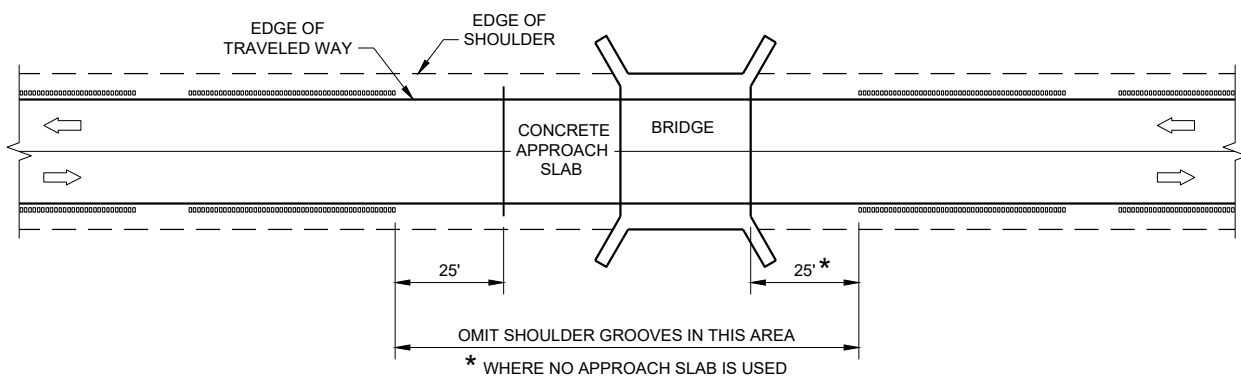
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



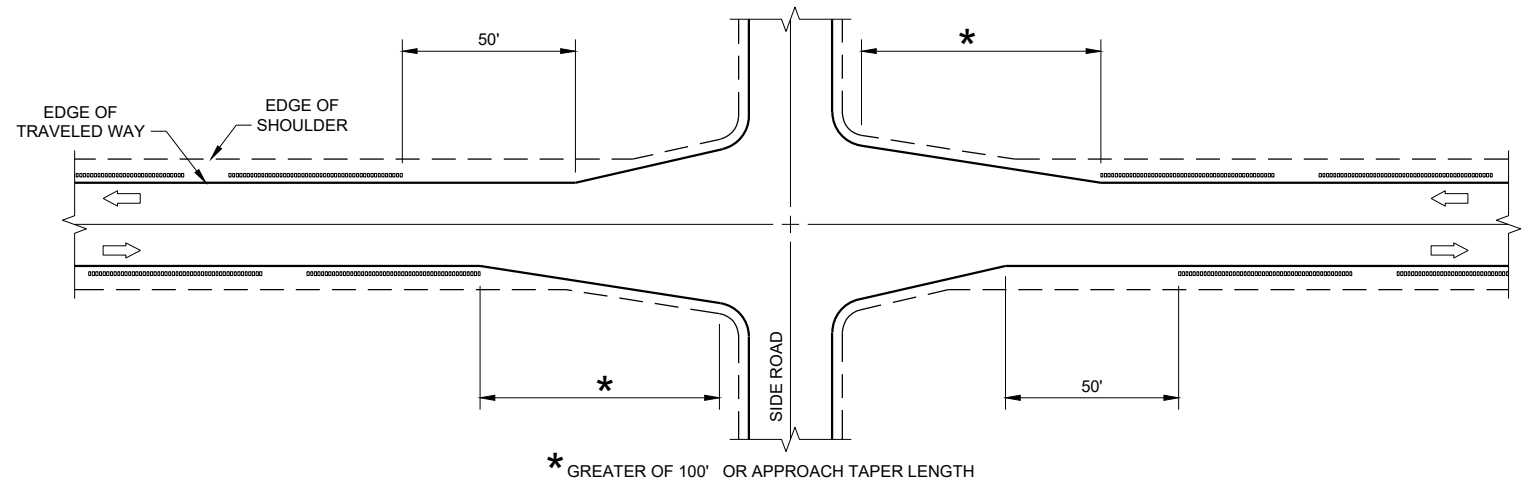
GROOVES AT MISCELLANEOUS CROSSINGS



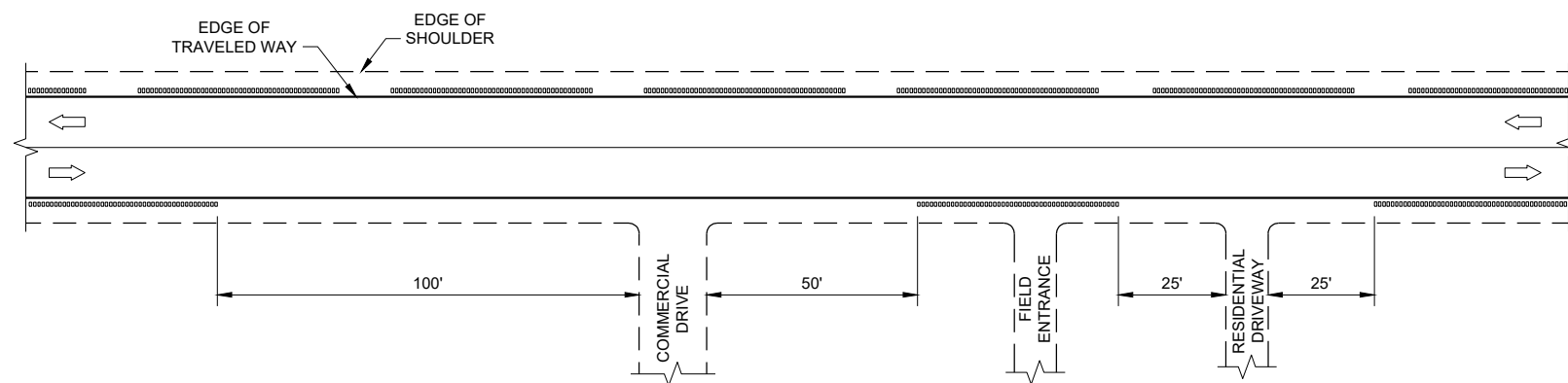
GROOVES AT RIGHT TURN LANE



GROOVES AT BRIDGES



GROOVES AT INTERSECTIONS WITH APPROACH TAPER



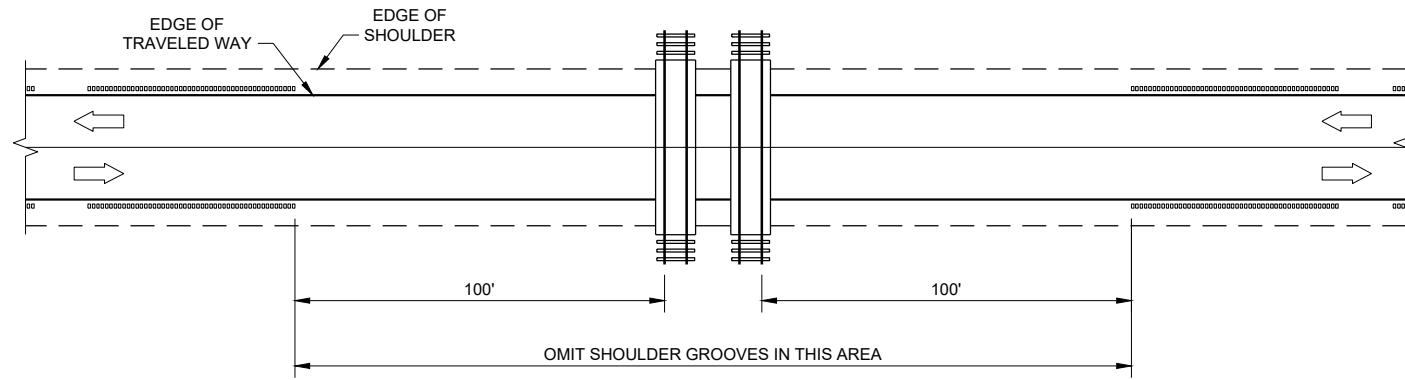
GROOVES AT DRIVEWAYS

GENERAL NOTES

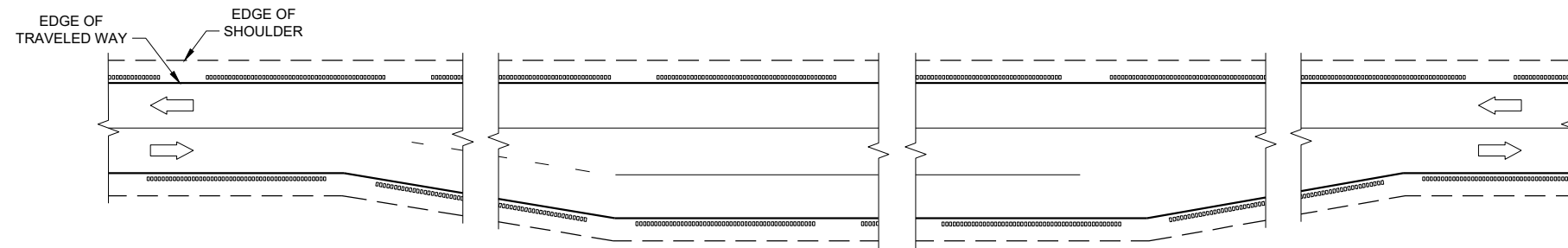
- ① SHOULDER GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS. WHEN DIRECTED BY THE ENGINEER.

**SHOULDER AND EDGE LINE
RUMBLE STRIPS
CROSSINGS, INTERSECTIONS,
BRIDGES, DRIVEWAYS**

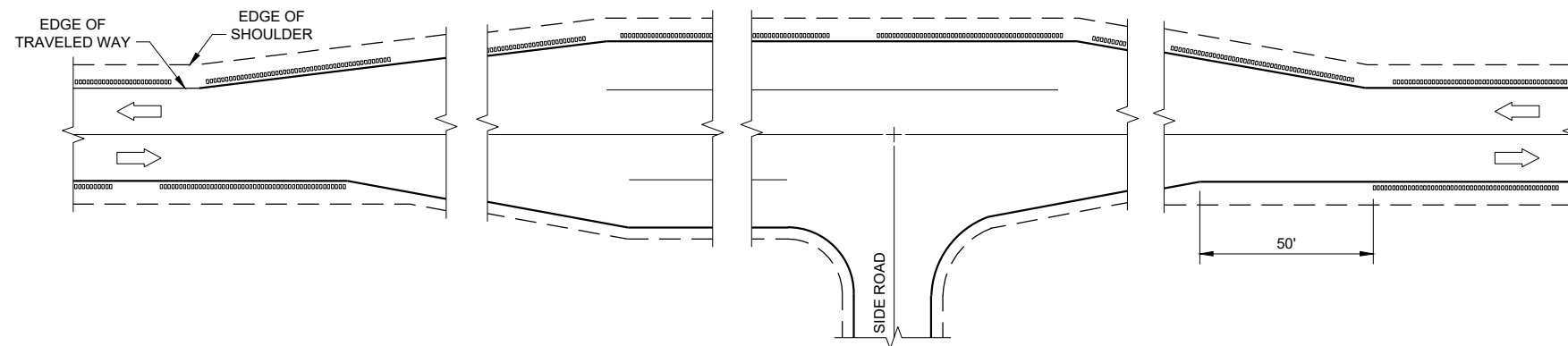
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



GROOVES AT RAILROADS



GROOVES AT PASSING AND CLIMBING LANES



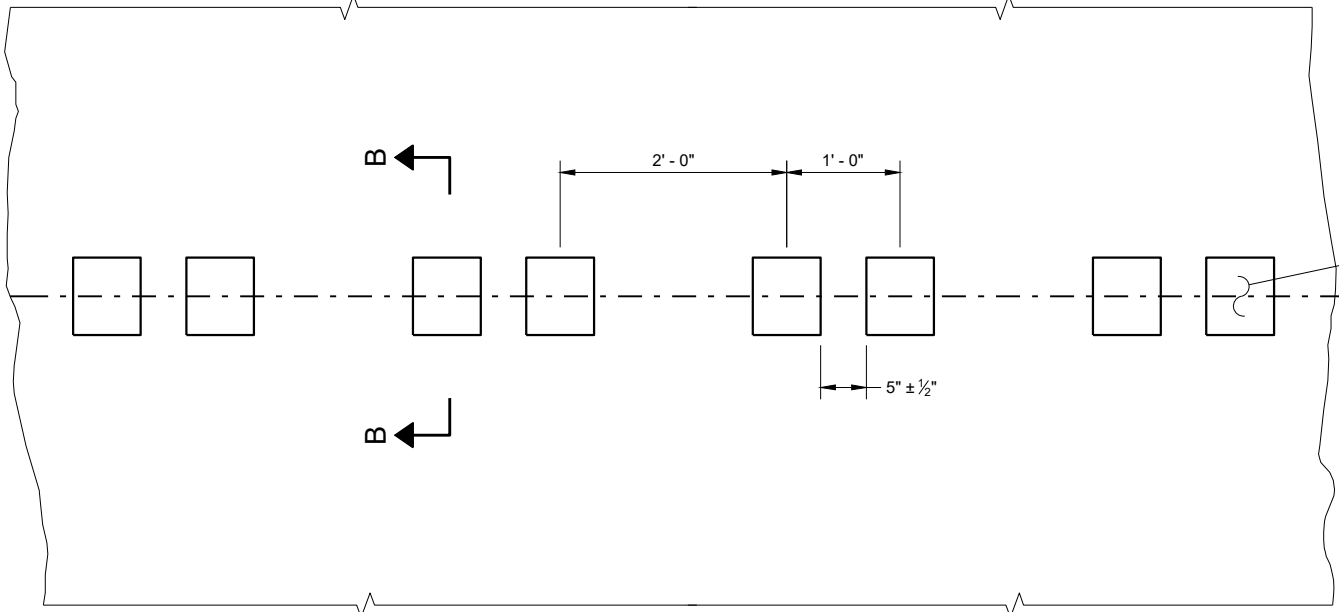
GROOVES AT BYPASS LANES

SHOULDER AND EDGE LINE RUMBLE STRIPS - RAILROAD, PASSING, CLIMBING AND BYPASS LANES	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2023 DATE	/S/ John Jenkins ROADWAY STANDARDS DEVELOPMENT ENGINEER
<small>FHWA</small>	

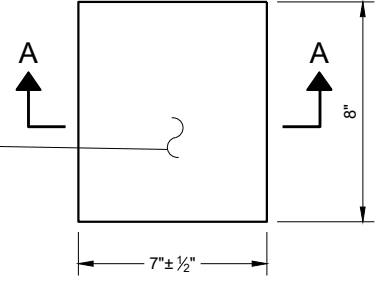
GENERAL NOTES

DO NOT MILL SHOULDER GROOVES THROUGH INTERSECTIONS, MARKED CROSSWALKS, NON-MOTORIZED PATH CROSSINGS, ETC. REFER TO SDD 13A11 SHEETS "d" AND "e".

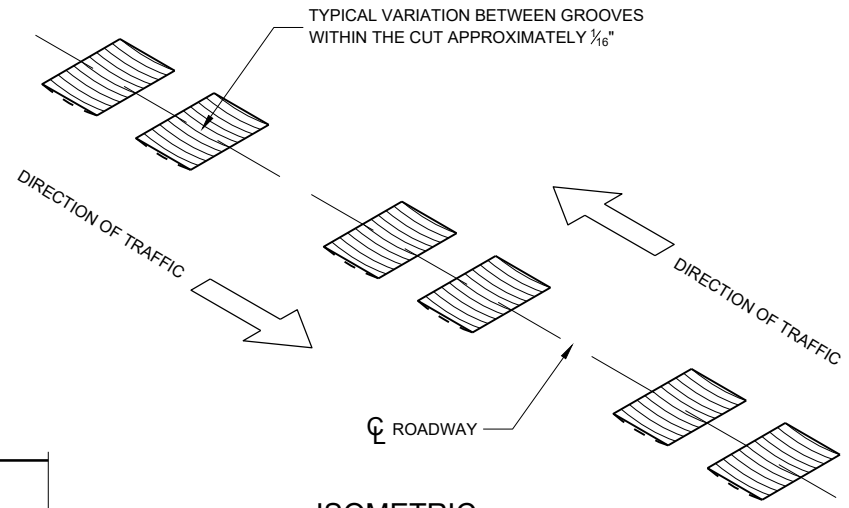
CENTERLINE GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS WHEN DIRECTED BY THE ENGINEER.



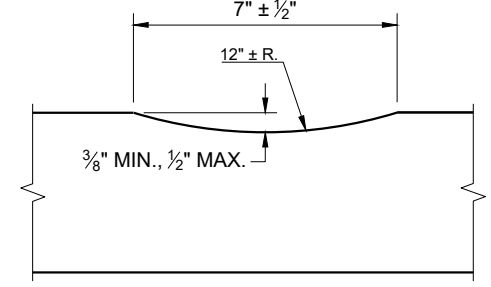
PLAN DETAIL VIEW



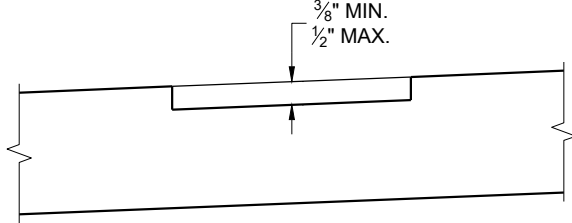
PLAN VIEW (SINGLE GROOVE)



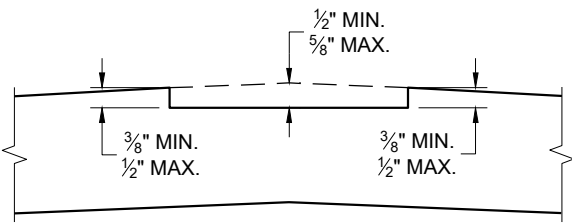
ISOMETRIC



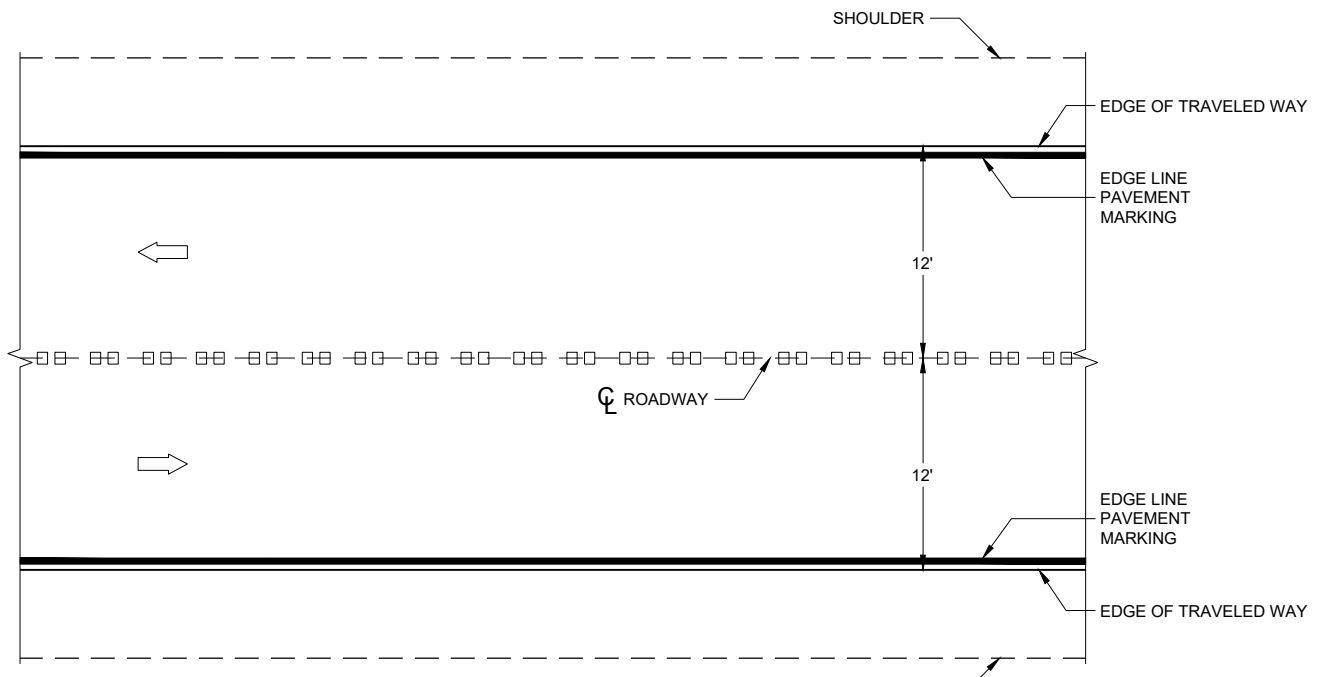
SECTION A - A



SECTION B - B SUPERELEVATED ROADWAY



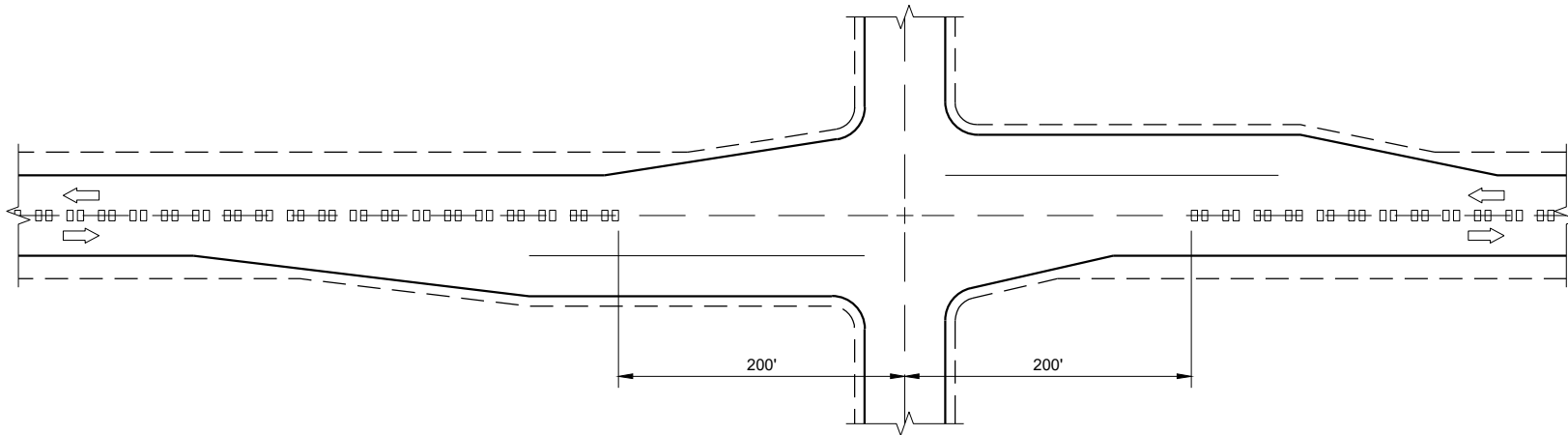
SECTION B - B CROWNED ROADWAY



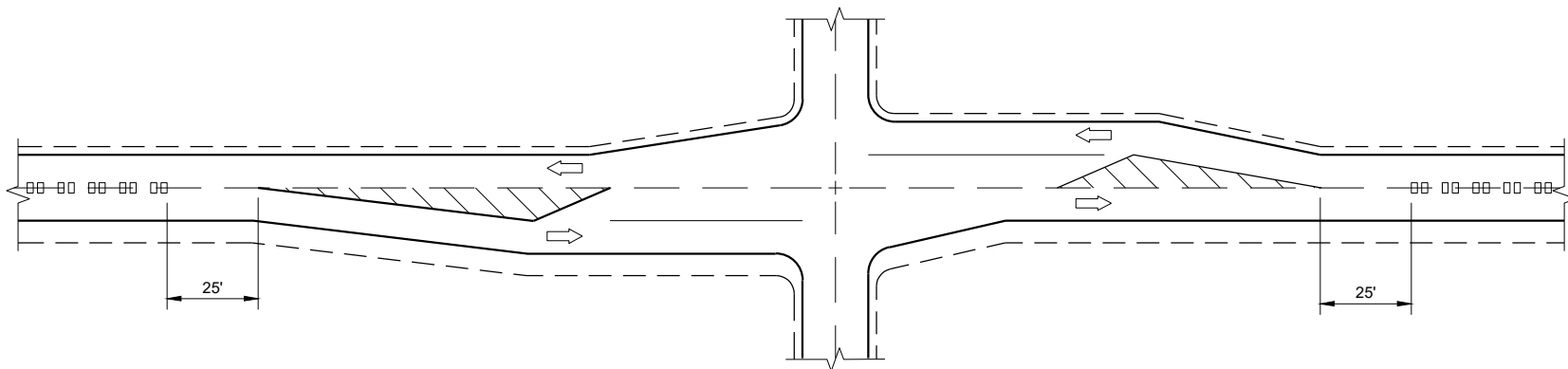
PLAN VIEW

CENTERLINE RUMBLE STRIPS - ASPHALT

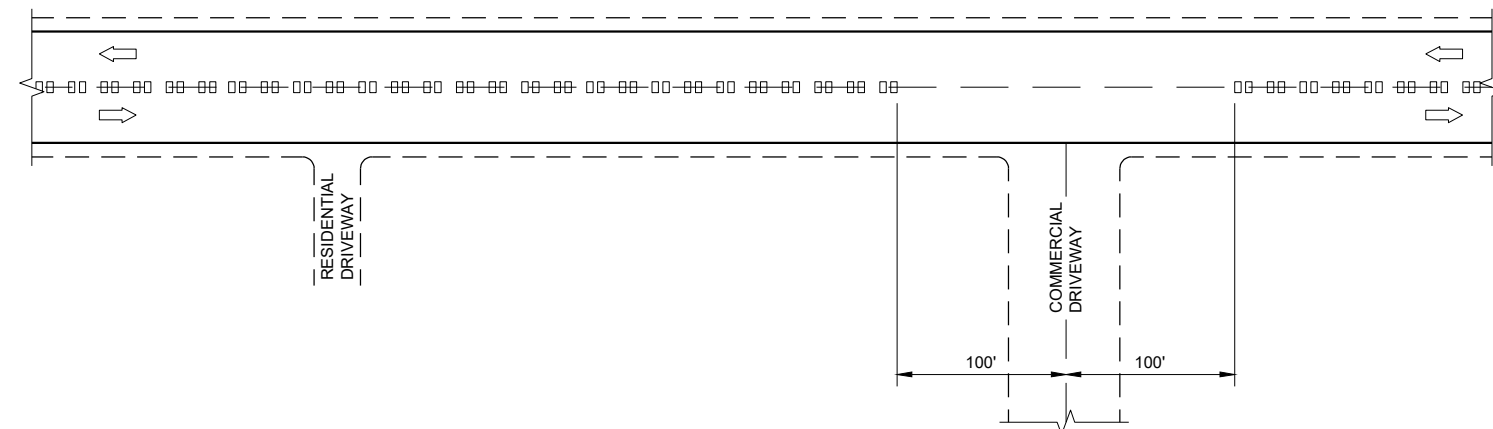
CENTERLINE RUMBLE STRIPS - ASPHALT
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION



CENTERLINE GROOVES AT INTERSECTIONS



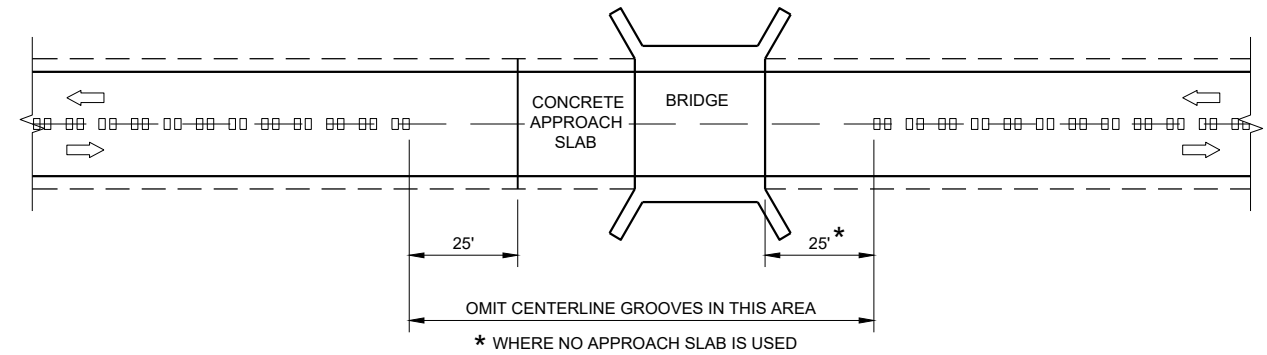
**CENTERLINE GROOVES AT INTERSECTIONS
(WITH LEFT TURN LANES)**



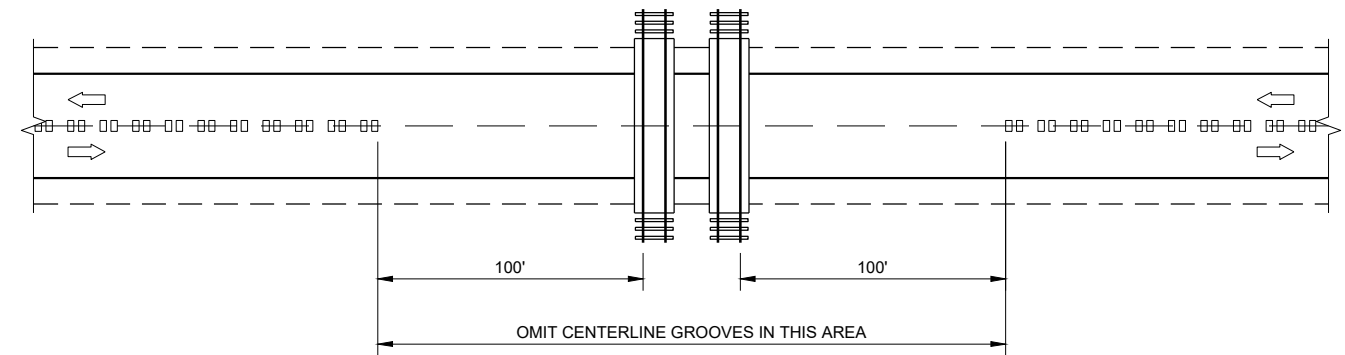
CENTERLINE GROOVES AT DRIVEWAYS^①

GENERAL NOTES

- ① CENTERLINE GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS WHEN DIRECTED BY THE ENGINEER.

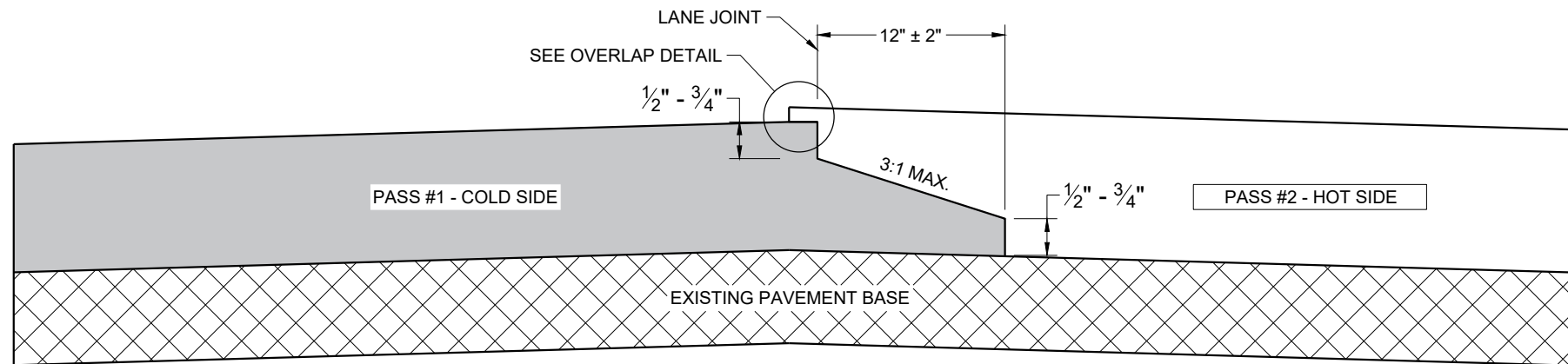


CENTERLINE GROOVES AT BRIDGES

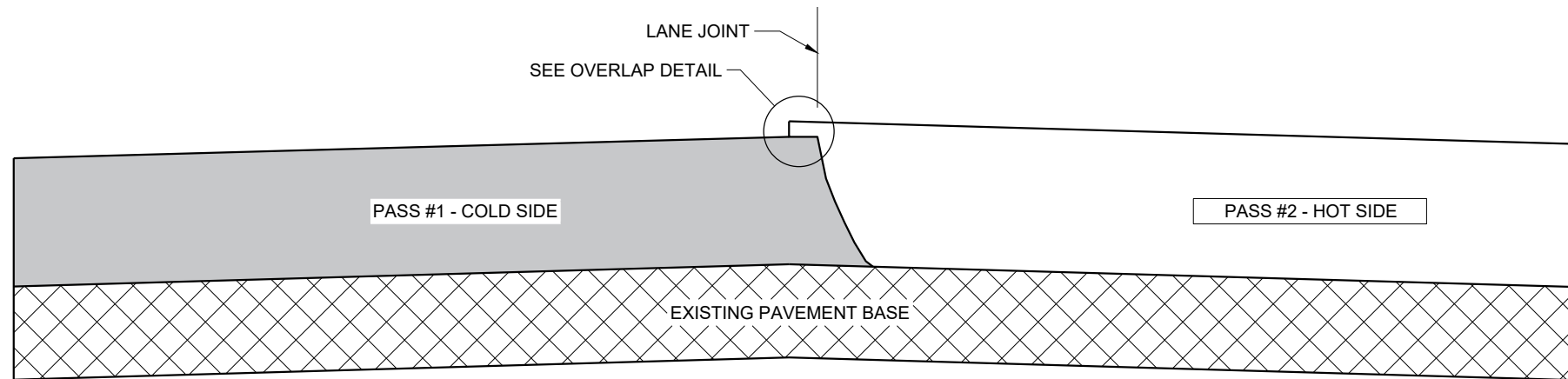


CENTERLINE GROOVES AT RAILROADS

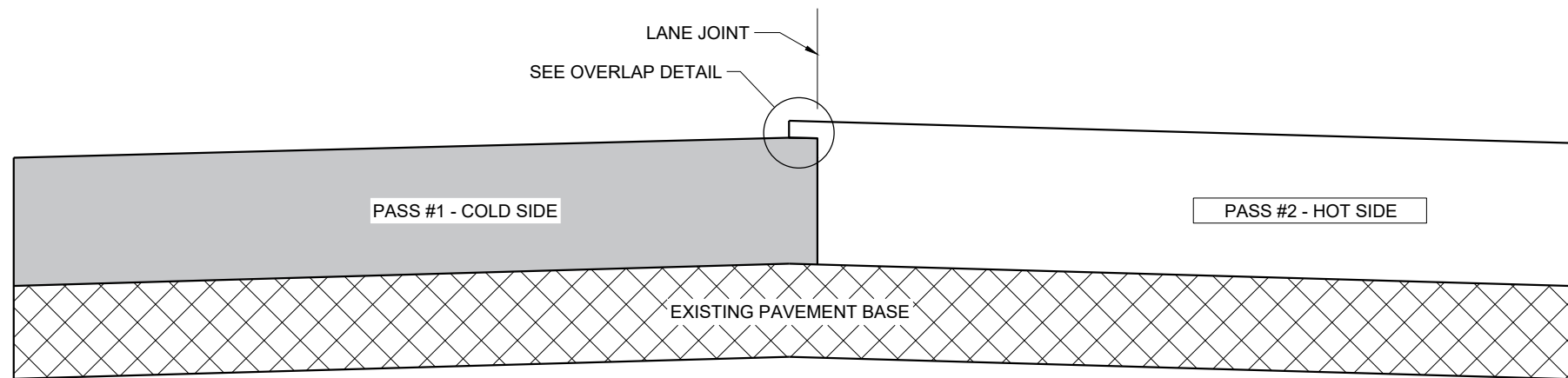
CENTER LINE RUMBLE STRIPS - INTERSECTIONS, DRIVEWAYS, BRIDGES, RAIL ROADS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2023 DATE	/S/ John Jenkins ROADWAY STANDARDS DEVELOPMENT ENGINEER
<small>FHWA</small>	



**TYPICAL PAVEMENT CROSS SECTION
NOTCHED WEDGE JOINT**



**TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT**



**TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT (MILLED)**

GENERAL NOTES

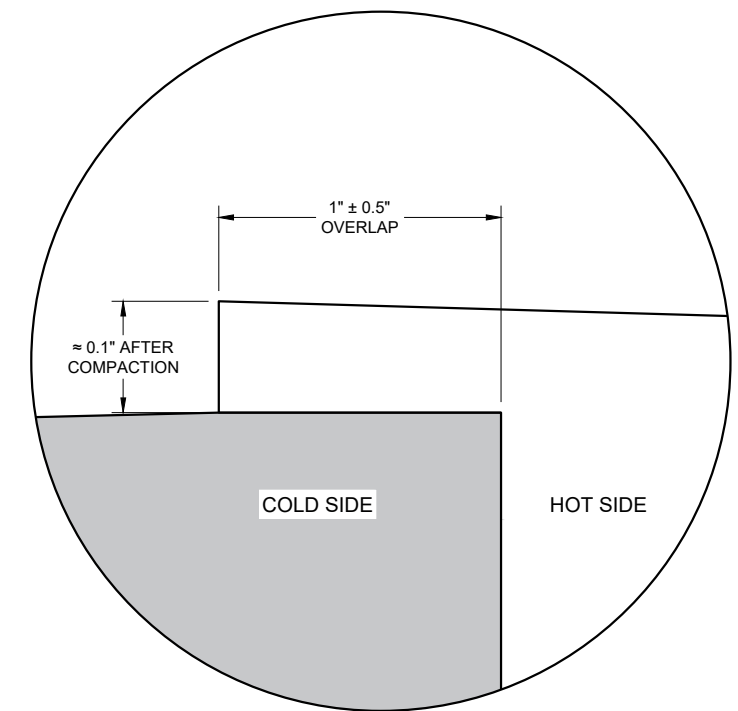
IN ADDITION TO THE DETAILS PROVIDED IN THIS DRAWING, CONFORM TO STANDARD SPECIFICATION 450.3.2.8 FOR WHEN A NOTCHED WEDGE JOINT IS REQUIRED AND FOR GENERAL JOINT CONSTRUCTION REQUIREMENTS.

FOR ALL LONGITUDINAL JOINTS, ENSURE THE PAVER SCREED OVERLAPS THE PREVIOUSLY PLACED PAVEMENT BY $1" \pm 0.5"$ AND THE HOT SIDE OF THE JOINT REMAINS HIGHER THAN THE COLD SIDE BY APPROXIMATELY 0.1" AFTER FINAL COMPACTION. (IT WILL BE FLUSH WHEN PAVING IN ECHELON.)

ONLY REMOVE THE LONGITUDINAL NOTCHED WEDGE JOINT FOR SMA PAVEMENT OR AS DIRECTED BY THE ENGINEER TO ADDRESS SPECIFIC LENGTHS OF JOINT DAMAGED BY TRAFFIC.

WHEN MILLING BACK OR REMOVING ANY LONGITUDINAL JOINT, LIMIT THE MATERIAL REMOVED TO 2" FROM THE TOP NOTCH OR FROM THE VERTICAL JOINT EDGE ON THE COLD SIDE OF THE JOINT.

USE LONGITUDINAL MILLED JOINT AS PLANS SHOW OR THE AS THE ENGINEER DIRECTS.



OVERLAP DETAIL (TYPICAL)

6

6

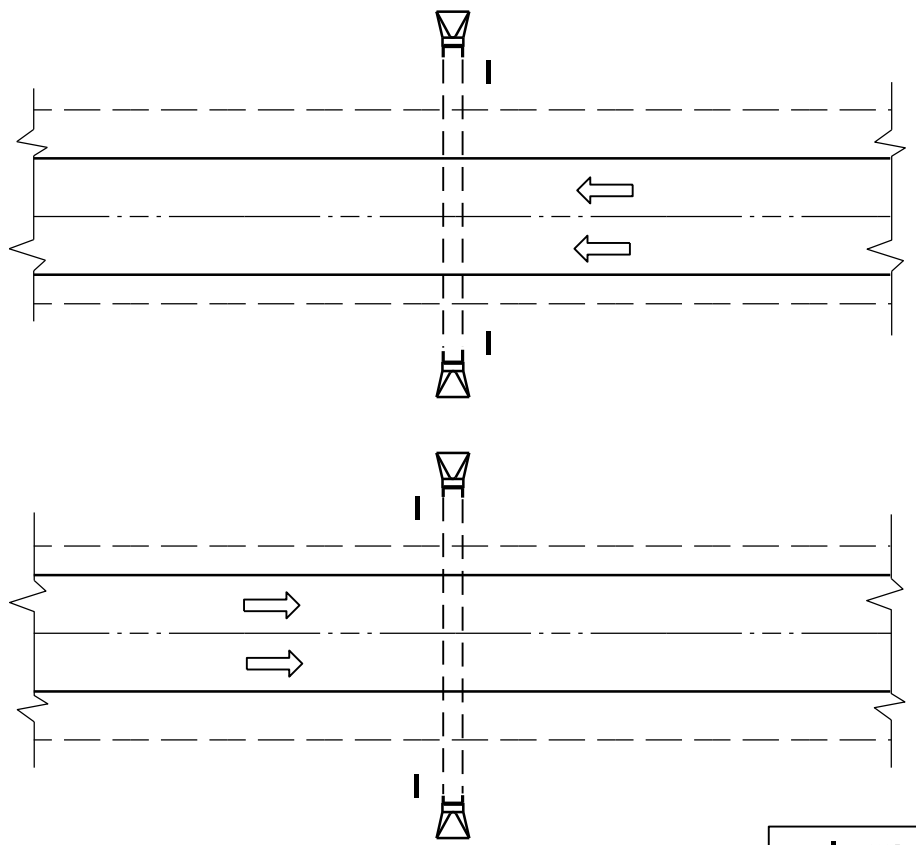
SDD 13C19 - 03

SDD 13C19 - 03

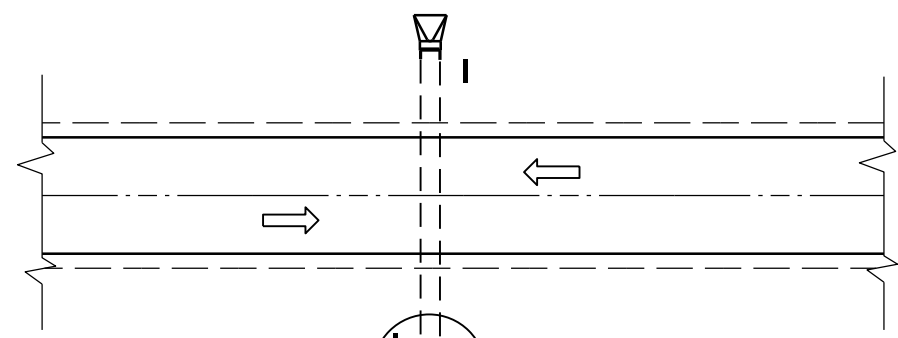
HMA LONGITUDINAL JOINTS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

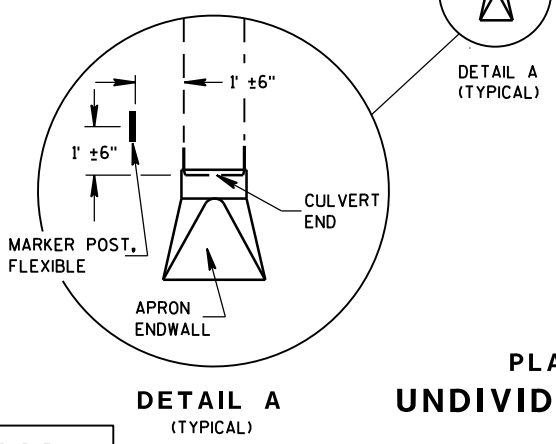
APPROVED
November 2020 DATE /S/ Steven Hefel
HMA PAVEMENT ENGINEER
FHWA



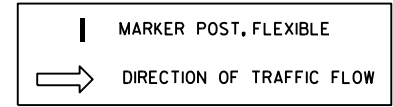
PLAN VIEW
DIVIDED HIGHWAY



PLAN VIEW
UNDIVIDED HIGHWAY

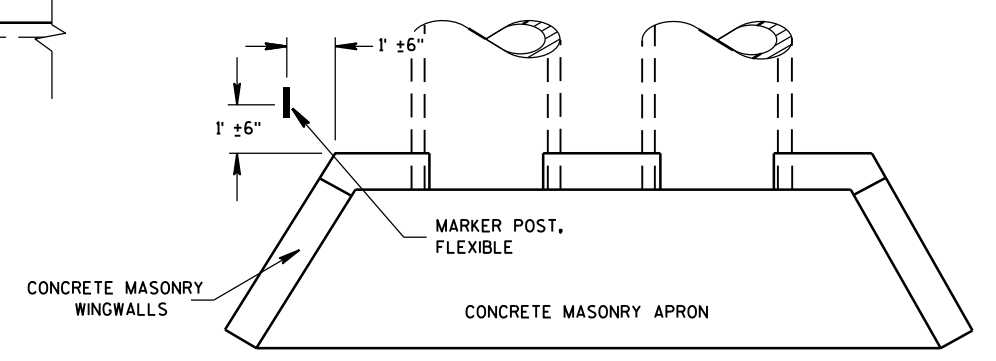


DETAIL A
(TYPICAL)



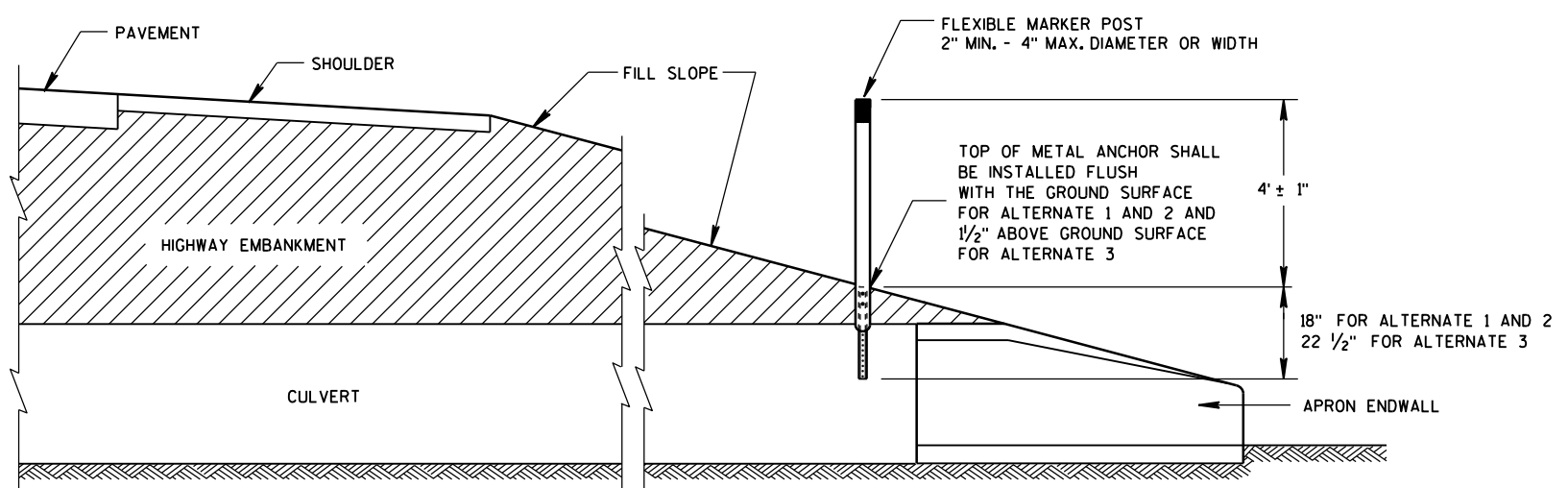
GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.



PLAN VIEW
CONCRETE MASONRY ENDWALLS FOR
CULVERT PIPE AND PIPE ARCH

FLEXIBLE MARKER POST LOCATION



CROSS SECTION
FLEXIBLE MARKER POST

**FLEXIBLE MARKER POST
FOR CULVERT END**

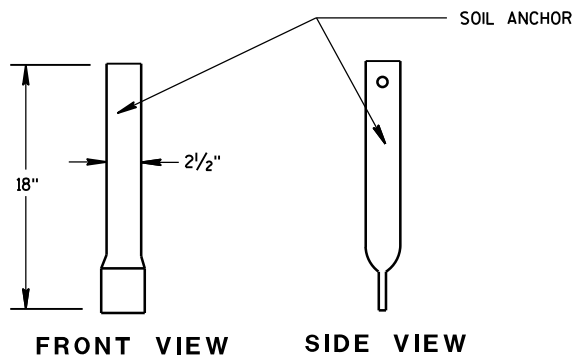
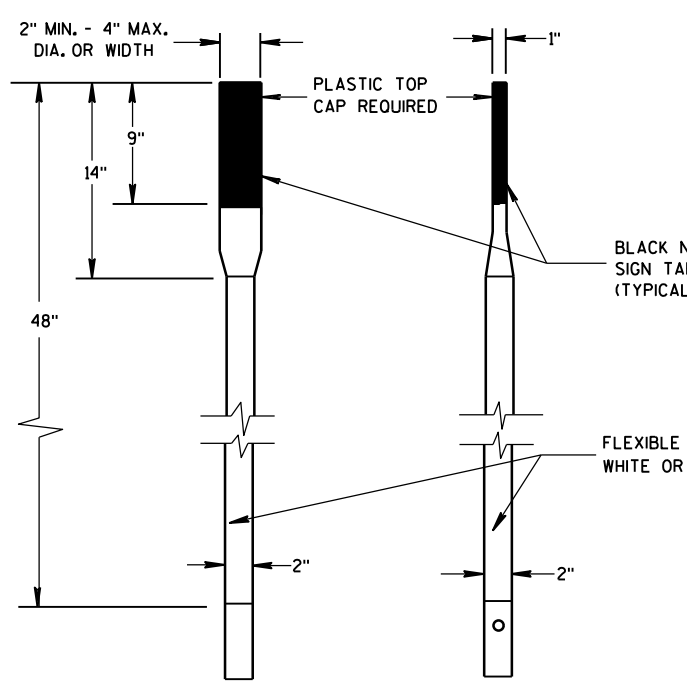
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

6

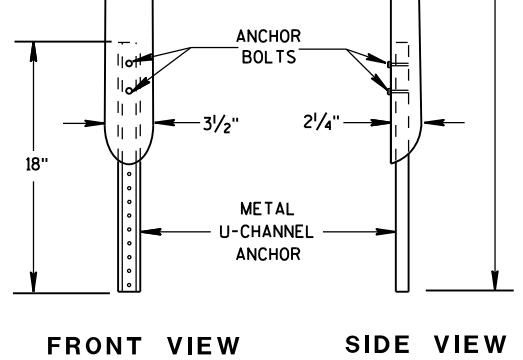
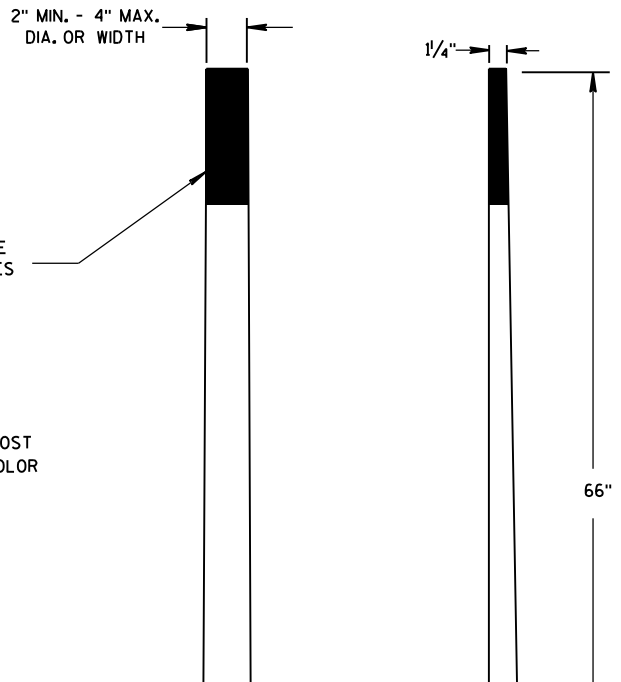
6

S.D.D. 15 A 3-2a

S.D.D. 15 A 3-2a

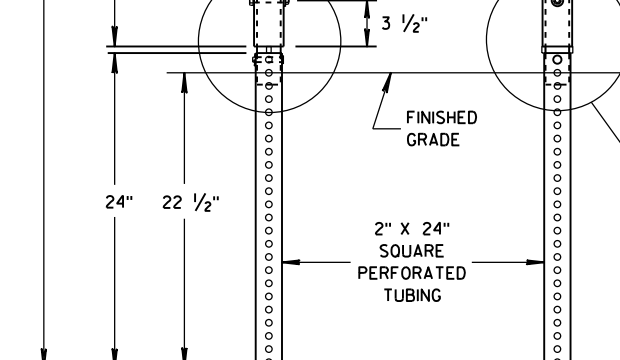
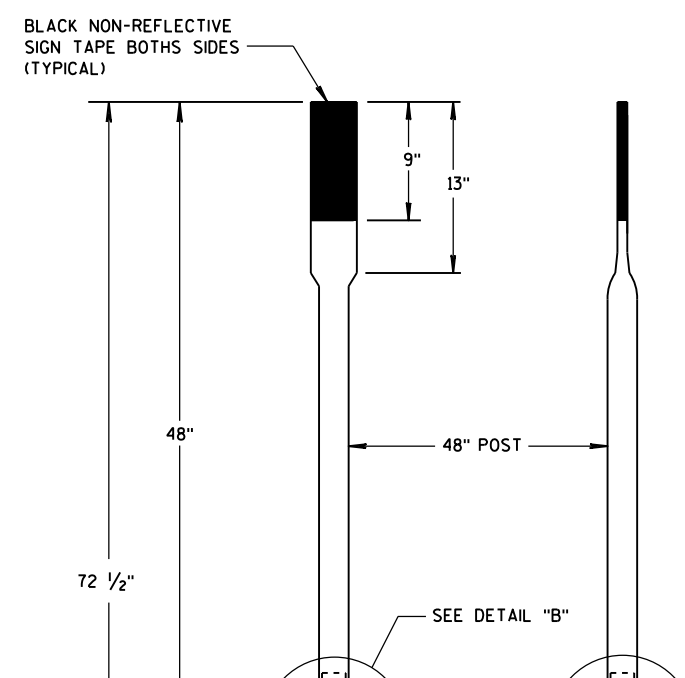


FRONT VIEW SIDE VIEW
ALTERNATE 1

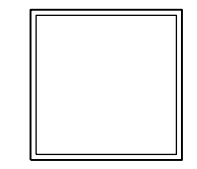


FRONT VIEW SIDE VIEW
ALTERNATE 2

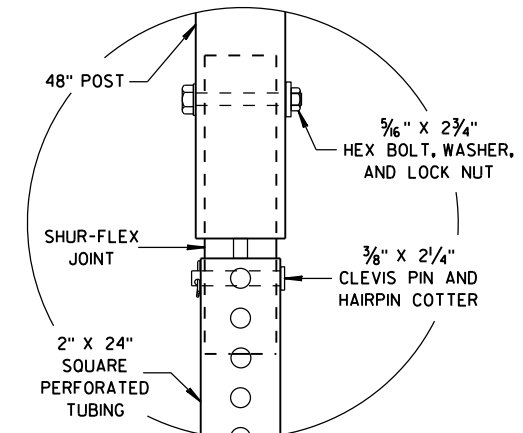
FLEXIBLE MARKER POSTS



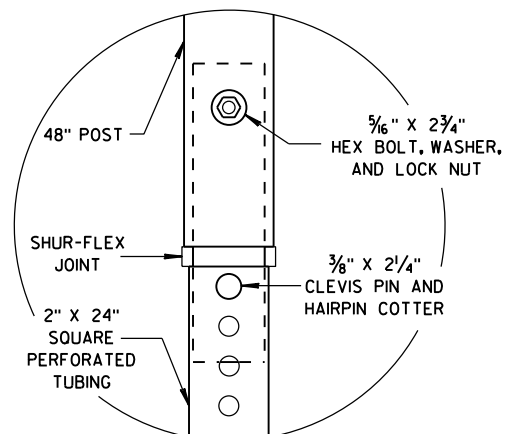
FRONT VIEW SIDE VIEW
ALTERNATE 3



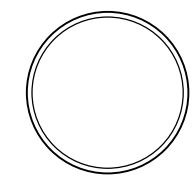
SECTION C-C



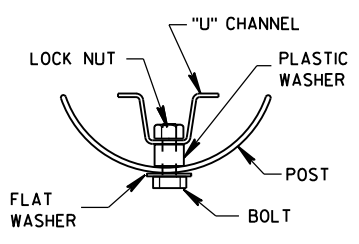
DETAIL B



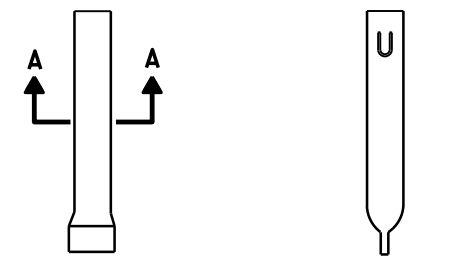
DETAIL C



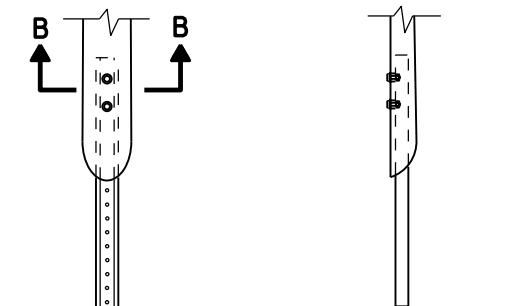
SECTION A-A



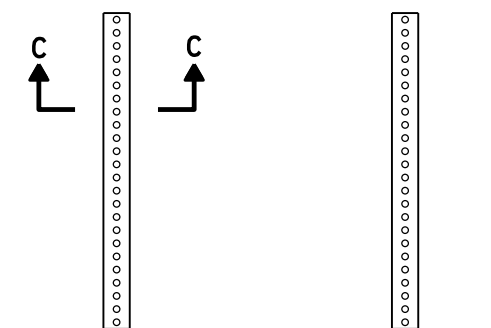
SECTION B-B



FRONT VIEW SIDE VIEW
ALTERNATE 1



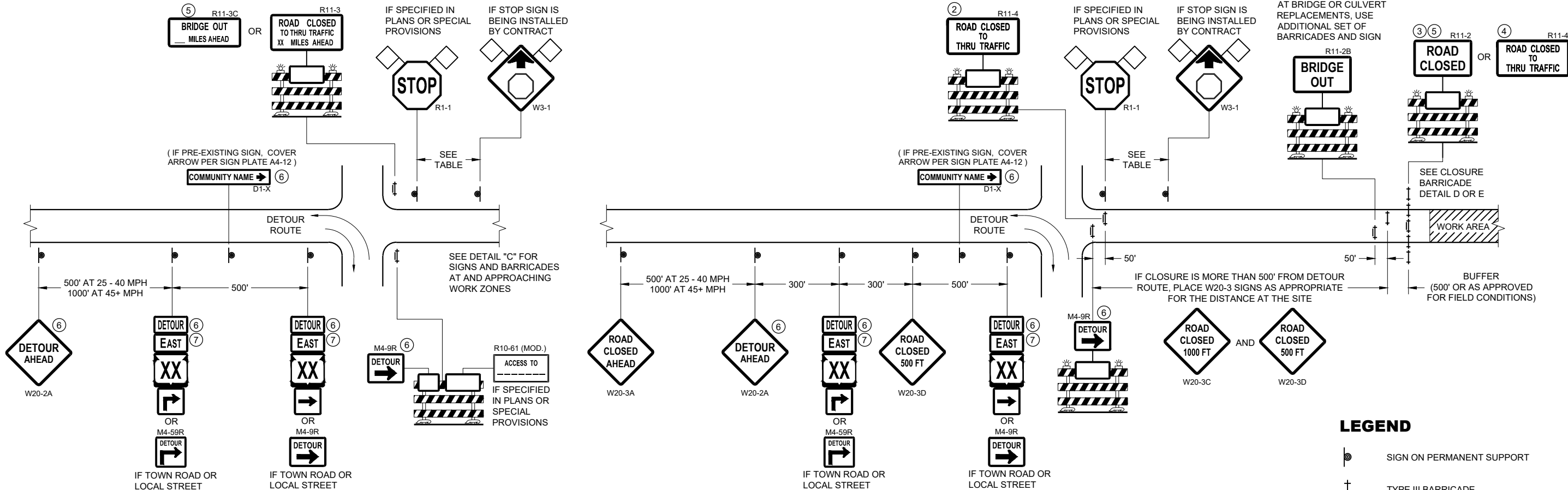
FRONT VIEW SIDE VIEW
ALTERNATE 2



FRONT VIEW SIDE VIEW
ALTERNATE 3

FLEXIBLE MARKER POST ANCHORS

FLEXIBLE MARKER POST FOR CULVERT END	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 10/1/2012 DATE	/S/ Travis Feltes STATE TRAFFIC ENGINEER OF DESIGN
FHWA	



**DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR**

WORK ZONE GREATER THAN OR EQUAL TO 1/2 MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)

**DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR**

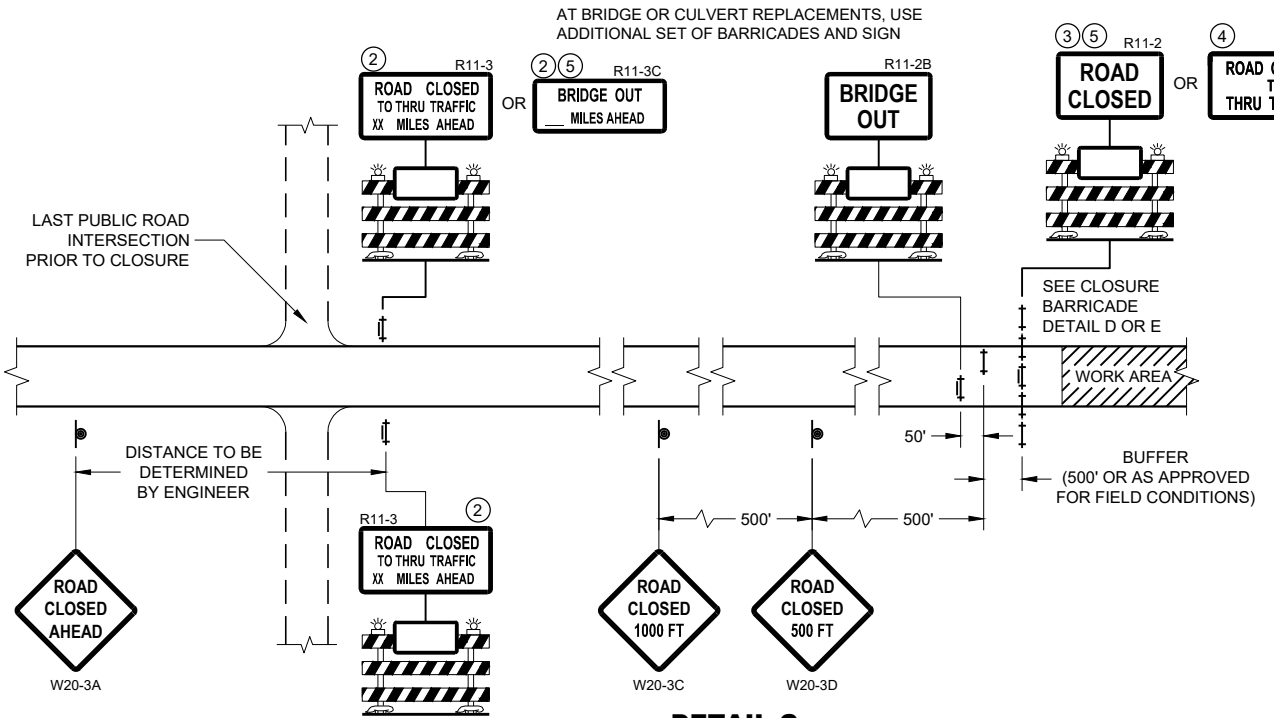
WORK ZONE LESS THAN 1/2 MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
- FLAGS, 16" X 16" MIN. (ORANGE)

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

- M4 - 8
- M3 - X
- M1 - 4
- M1 - 6
- M1 - 5A
- M05 - 1
- M06 - 1



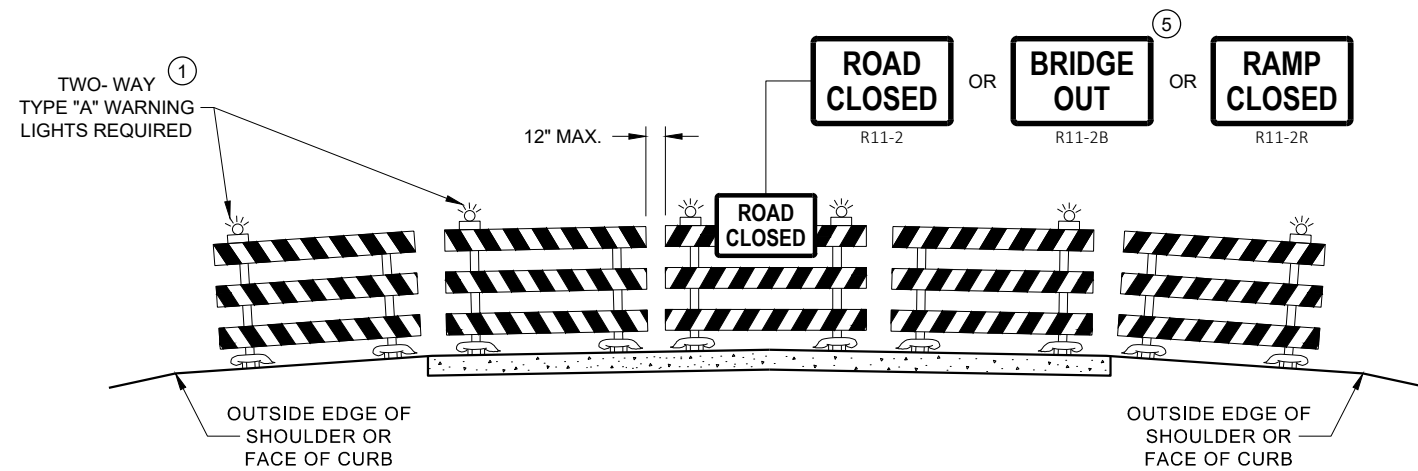
**DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR**

SEE SDD 15C2-SHEET "b"
FOR GENERAL NOTES
AND FOOTNOTES ① THROUGH ⑦

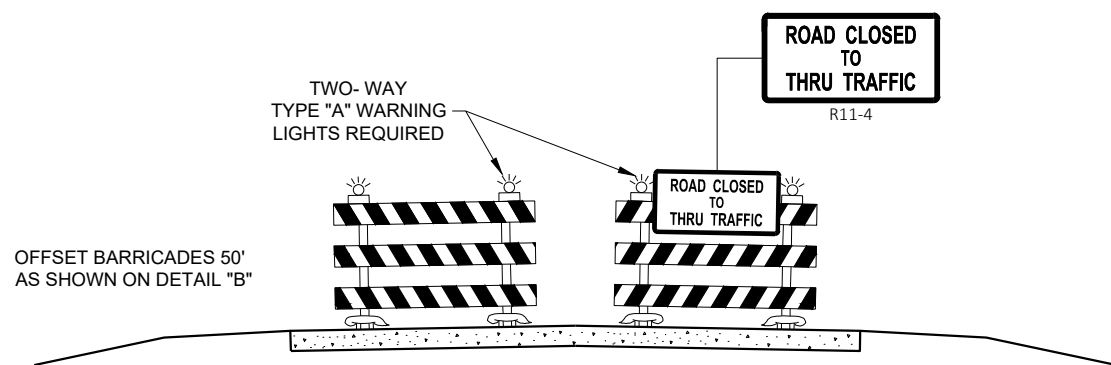
**BARRICADES AND SIGNS
FOR MAINLINE CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 /S/ Andrew Heidtke
DATE DATE WORK ZONE ENGINEER
FHWA



**DETAIL D
ROAD CLOSURE BARRICADE DETAIL
APPROACH VIEW**



**DETAIL E
LANE CLOSURE BARRICADE DETAIL
APPROACH VIEW**

SEE SDD 15C2 - SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW - INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11 - 2, R11 - 3, M4 - 9, R11 - 4, AND R10 - 61 SIGNS PLACED ON THE BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE RAIL OR BOTTOM RAILS.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

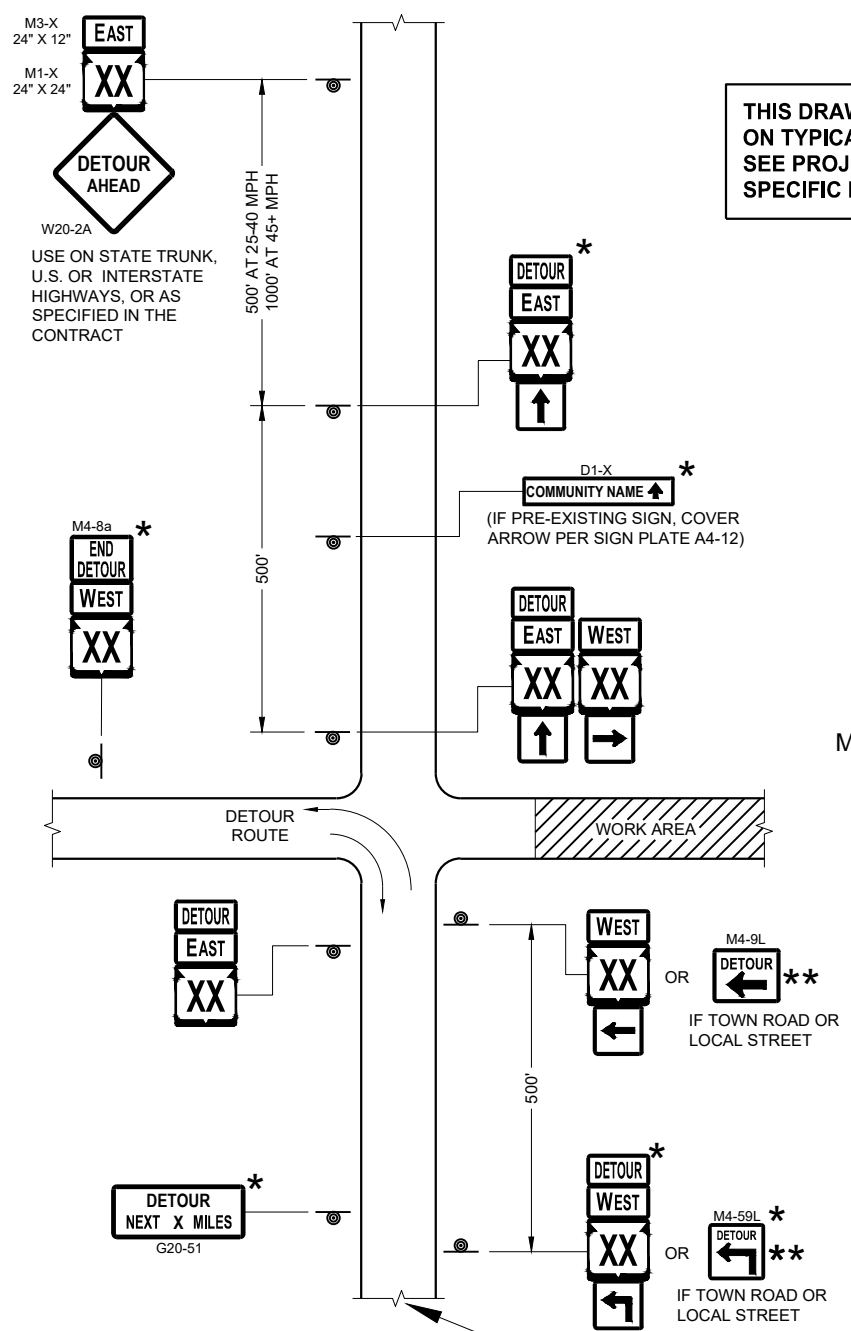
- R11 - 2 SHALL BE 48" X 30"
- R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60" X 30"
- M4 - 9 SHALL BE 30" X 24"
- M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1 - 1 SHALL BE 36" X 36"

- ① TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT SPACING).
- ② THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.
- ③ FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".
- ④ FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".
- ⑤ FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.
- ⑥ INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.
- ⑦ "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

**BARRICADES AND SIGNS
FOR
VARIOUS CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER



THIS DRAWING PROVIDES GENERAL GUIDANCE ON TYPICAL DETOUR SIGN LAYOUT AND SPACING. SEE PROJECT DETOUR SIGNING SHEETS FOR SPECIFIC DETAILS FOR EACH PROJECT.

LEGEND

- SIGN ON PERMANENT SUPPORT
- WORK AREA
- M4 - 8
- M3 - X
- M1 - 4
- M1 - 6
- M1 - 5A
- M05 - 1
- M06 - 1
- M06 - 1

GENERAL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. MODIFY EXISTING SIGNS WHERE POSSIBLE.

THE SPACING BETWEEN TRAFFIC CONTROL AND DETOUR SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

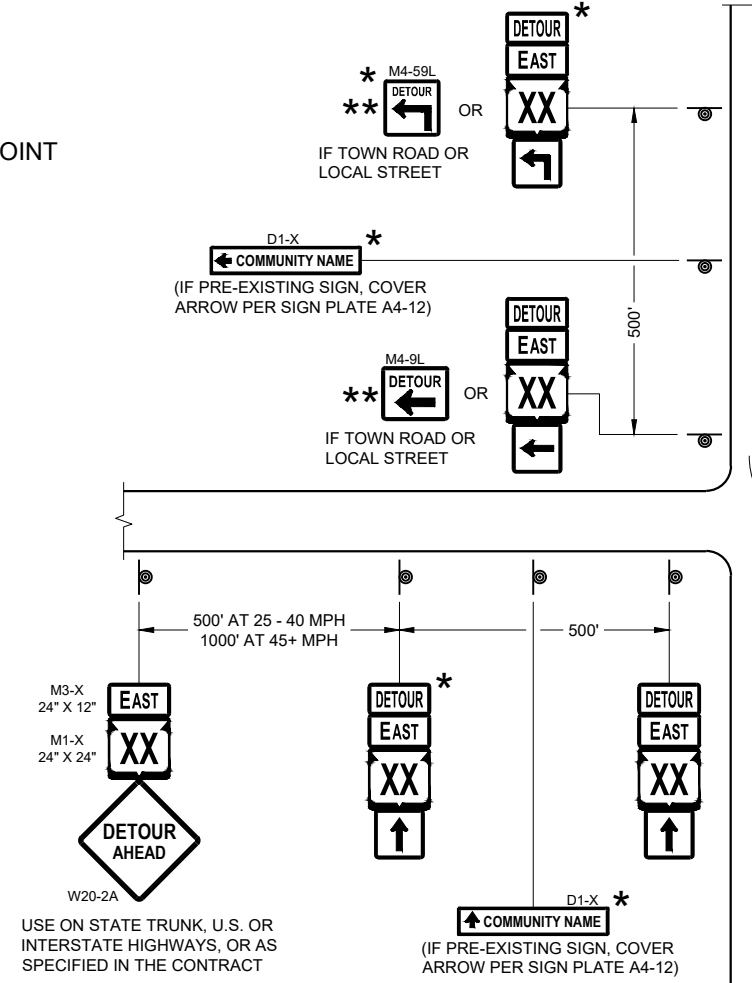
"MO" SIGNS ARE THE SAME AS "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGN SIZES SHALL BE AS FOLLOWS:

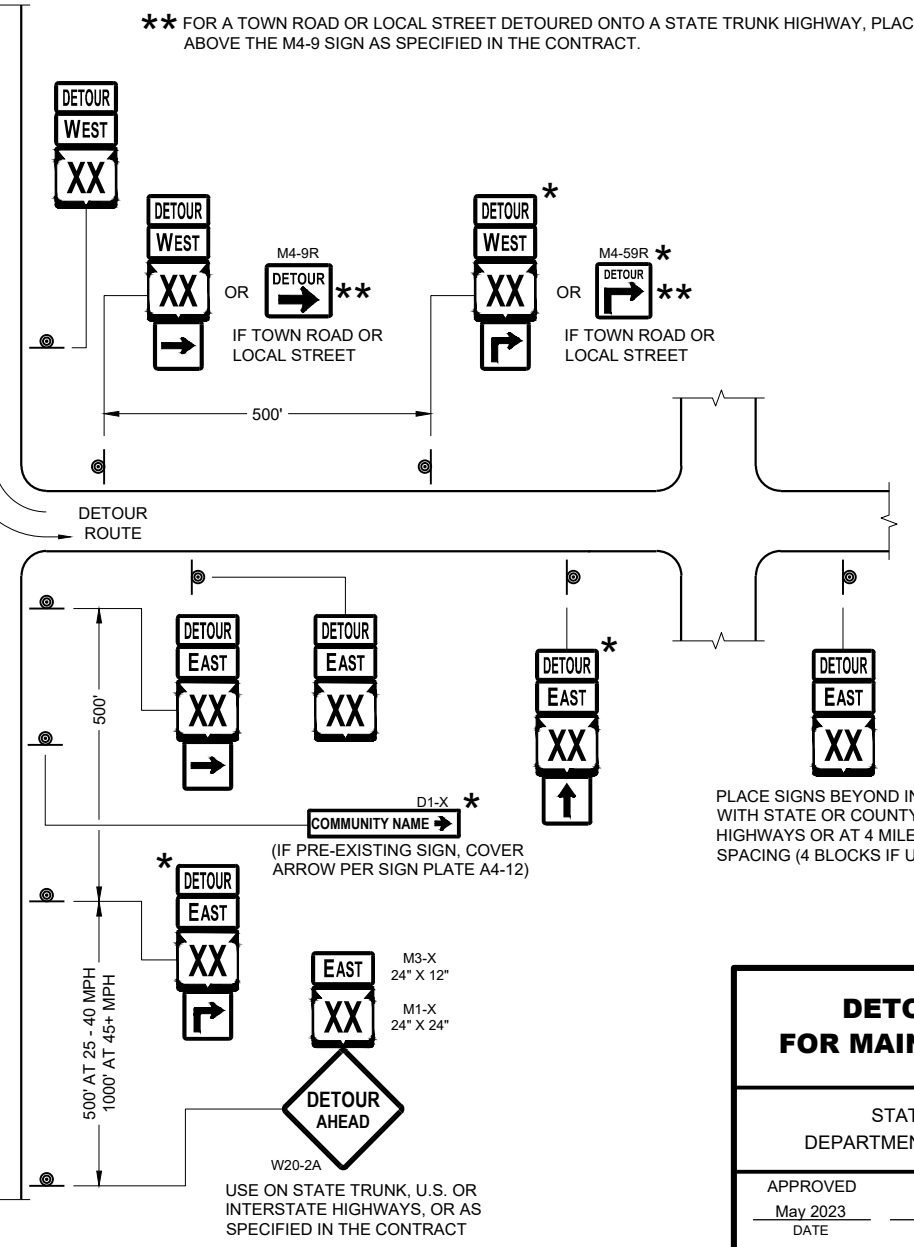
- M3-X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4-8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1-4, M1-5A AND M1-6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- M05-1 AND M06-1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- M4-9 AND M4-59 SHALL BE 30" X 24"
- M4-8a SHALL BE 24" X 18"
- G20-51 SHALL BE 60" X 24"
- W20-2A SHALL BE 48" X 48"
- D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.

- * OPTIONAL SIGNS. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS.
- ** FOR A TOWN ROAD OR LOCAL STREET DETOURED ONTO A STATE TRUNK HIGHWAY, PLACE A ROAD NAME PLAQUE ABOVE THE M4-9 SIGN AS SPECIFIED IN THE CONTRACT.

MATCH POINT



**DETAIL F
DETOUR SIGNING**



PLACE SIGNS BEYOND INTERSECTIONS WITH STATE OR COUNTY TRUNK HIGHWAYS OR AT 4 MILE MAXIMUM SPACING (4 BLOCKS IF URBAN AREA)

SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS AND DETAIL A OR B ON SDD SHEET 15C02 - SHEET "a"

DETOUR SIGNING FOR MAINLINE CLOSURES	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2023 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
<small>FHWA</small>	

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.




ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

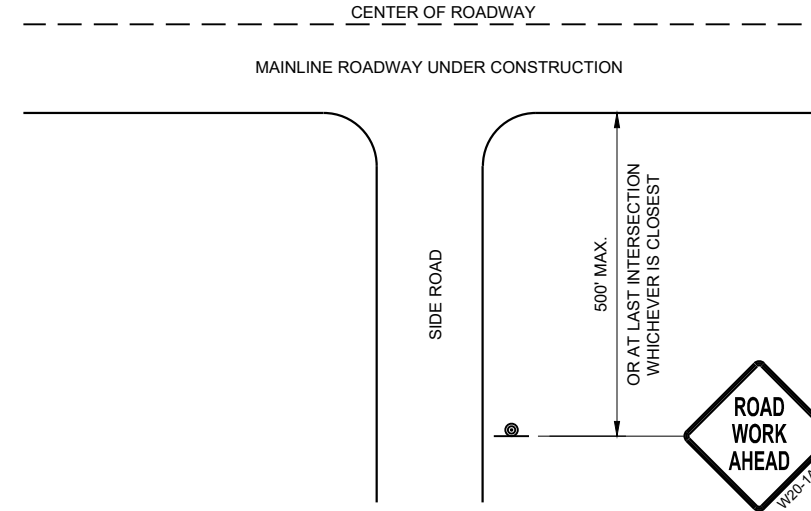
SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

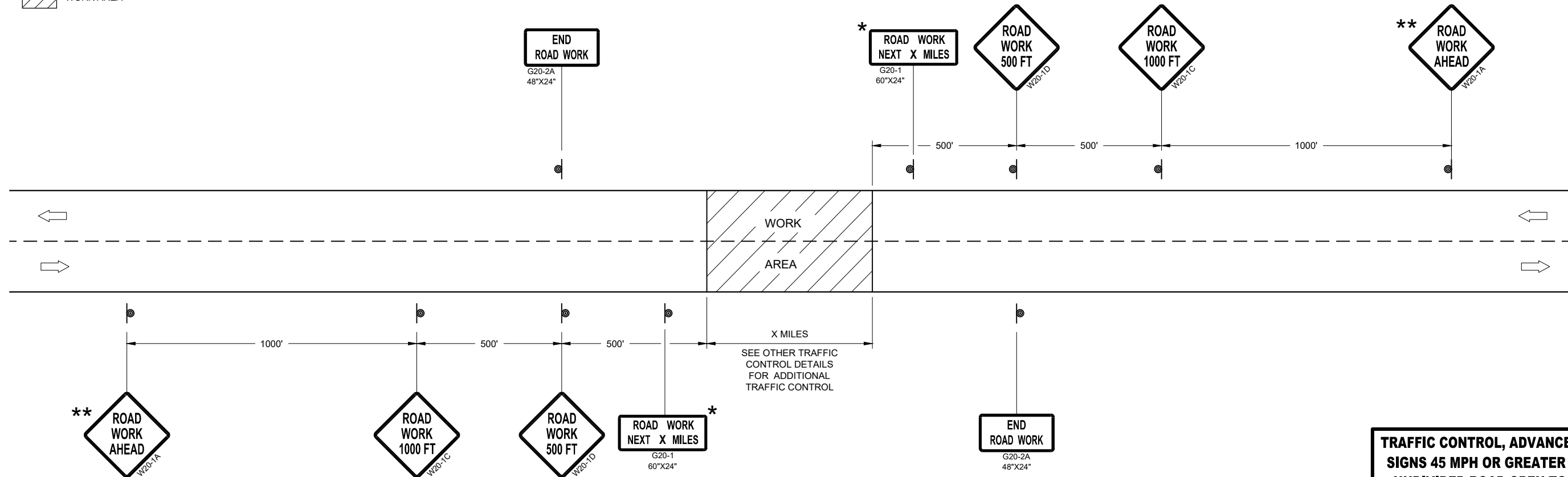
- * OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS
- ** PLACE AN ADDITIONAL W20-1A "ROAD WORK AHEAD" SIGN IF WORK AREA WITHIN THE PROJECT IS SEPARATED BY MORE THAN 2 MILES FROM PREVIOUS WORK AREA.

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC
-  WORK AREA



**TYPICAL SIDE ROAD APPROACH
WARNING SIGN DETAIL**



TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45MPH OR GREATER

**TRAFFIC CONTROL, ADVANCE WARNING
SIGNS 45 MPH OR GREATER TWO-WAY
UNDIVIDED ROAD OPEN TO TRAFFIC**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED _____ /S/ Andrew Heidtke
DATE July 2018 WORK ZONE ENGINEER

FHWA

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

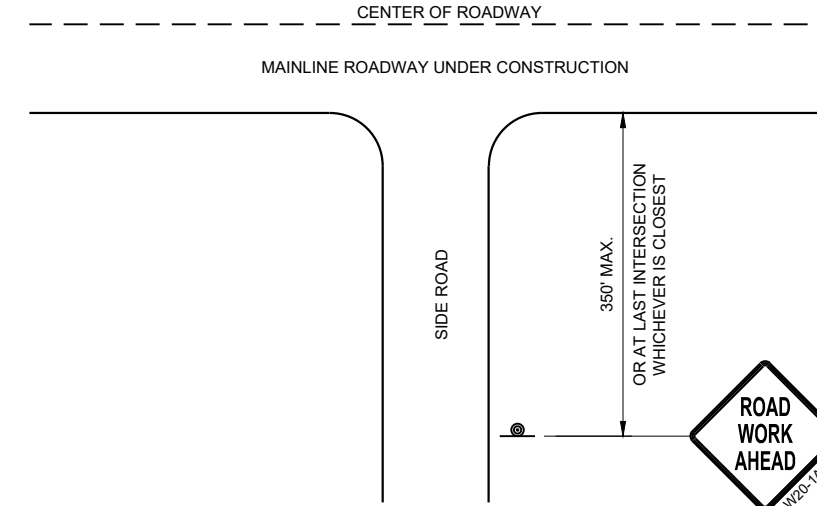
THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS, 36"X36" SIGNS MAY BE USED INSTEAD OF 48" X 48" SIGNS.

SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

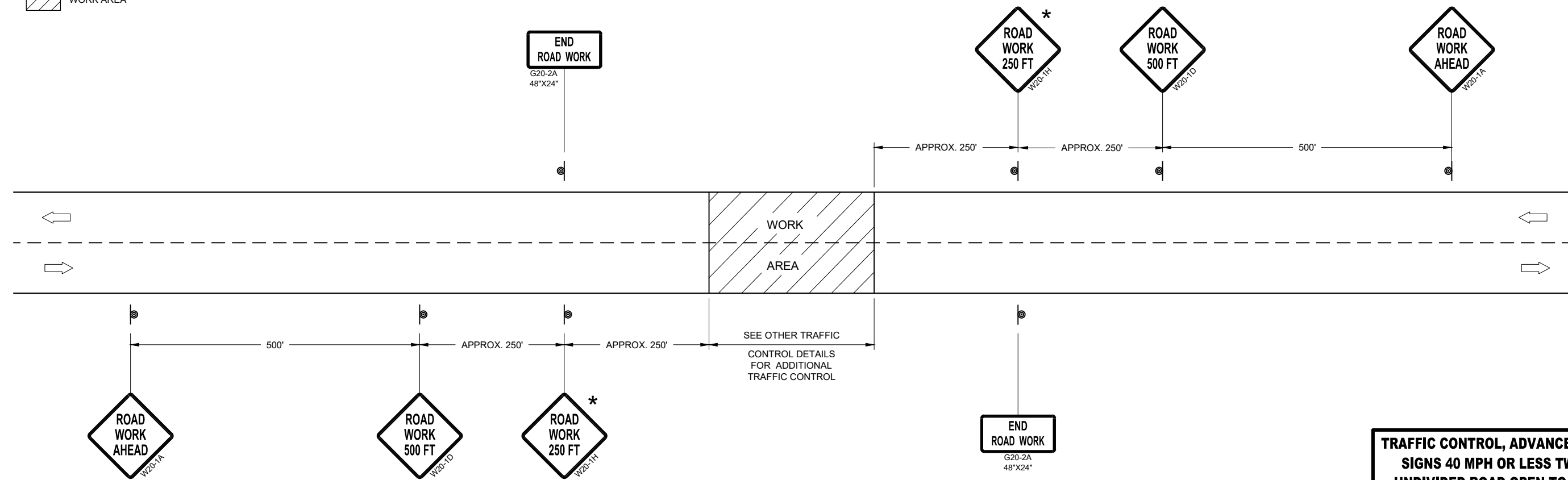
* THE THIRD W20-1 SIGN IS REQUIRED ONLY IF THERE IS AN INTERSECTION BETWEEN THE "ROAD WORK 500 FEET" SIGN AND THE WORK ZONE. ADJUST THE PLACEMENT OF THIS SIGN BASED ON INTERSECTION LOCATION AND OTHER FIELD CONDITIONS.



TYPICAL SIDE ROAD APPROACH WARNING SIGN DETAIL

LEGEND

- SIGN ON PERMANENT SUPPORT
- DIRECTION OF TRAFFIC
- WORK AREA



TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40MPH OR LESS

TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 MPH OR LESS TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED July 2018 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

GENERAL NOTES

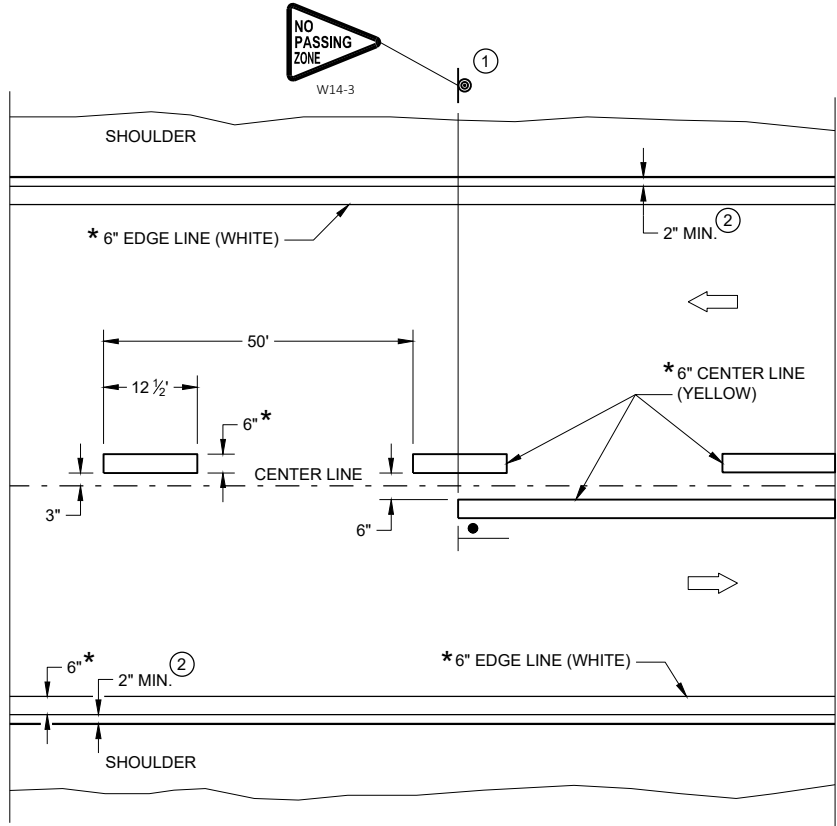
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

- ① LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING
- ② MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

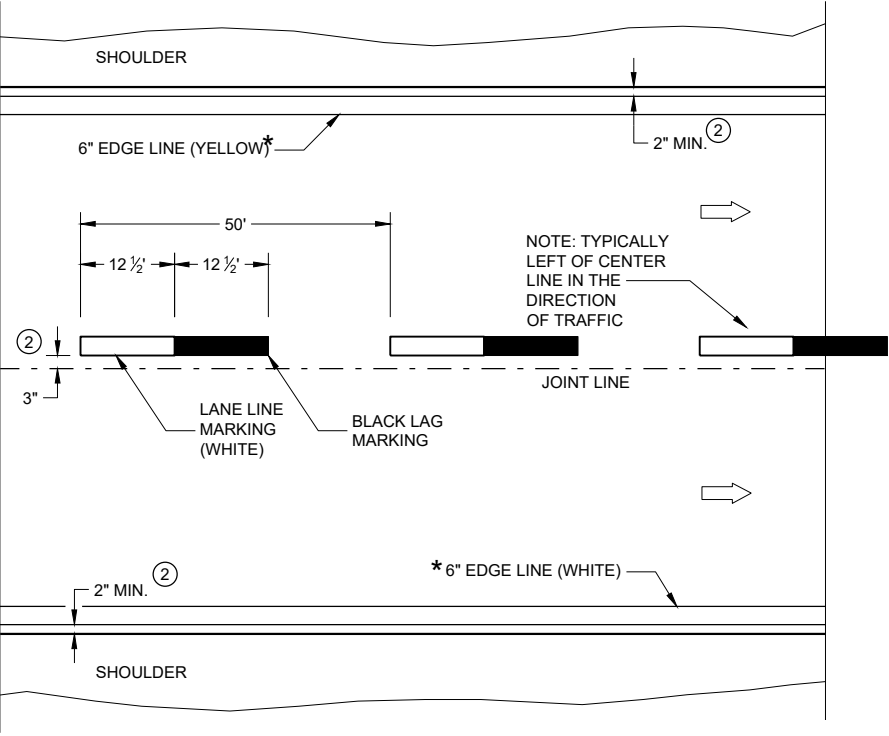
LEGEND

- ⊥ "T" MARKING
- ⊙ SIGN ON PERMANENT SUPPORT
- ➔ DIRECTION OF TRAFFIC

* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES



TWO WAY TRAFFIC



ONE WAY TRAFFIC

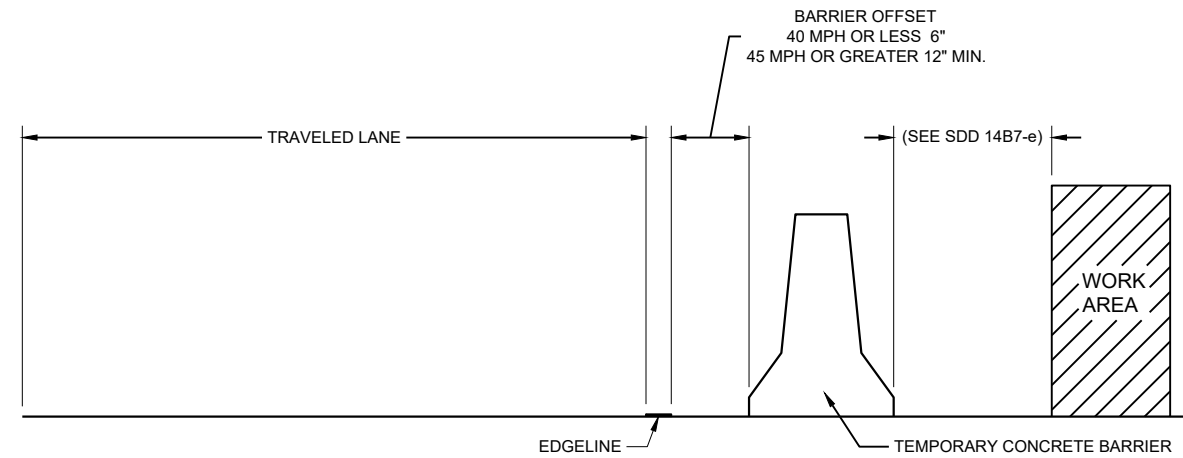
PERMANENT PAVEMENT MARKING

PERMANENT LONGITUDINAL PAVEMENT MARKINGS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
 May 2023 /S/ Jeannie Silver
 DATE STATEWIDE SIGNING AND MARKING ENGINEER

FHWA



TEMPORARY BARRIER OFFSET FROM EDGELINE

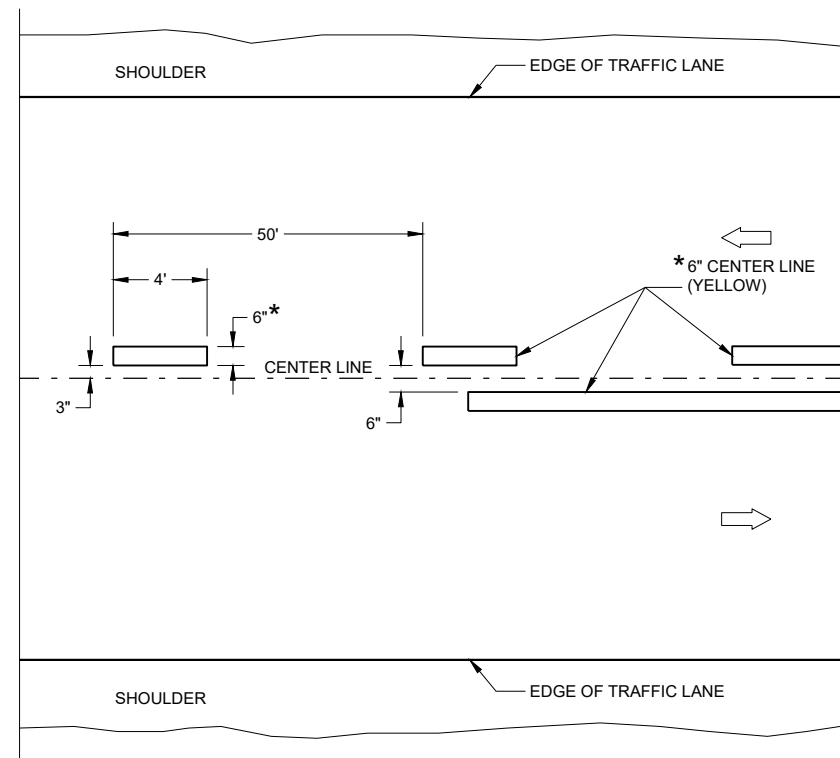
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

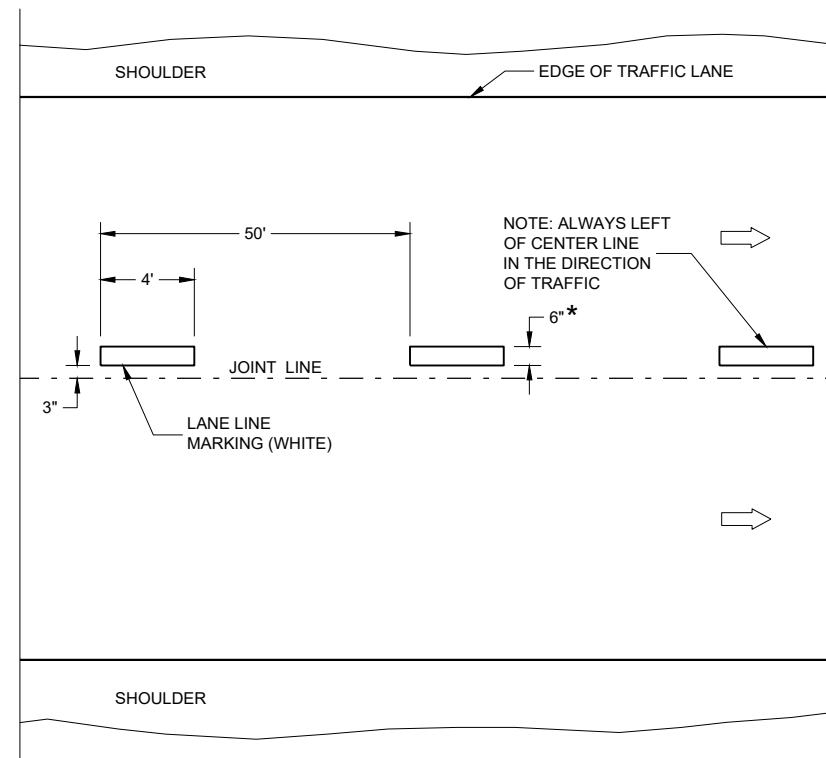
LEGEND

➡ DIRECTION OF TRAFFIC

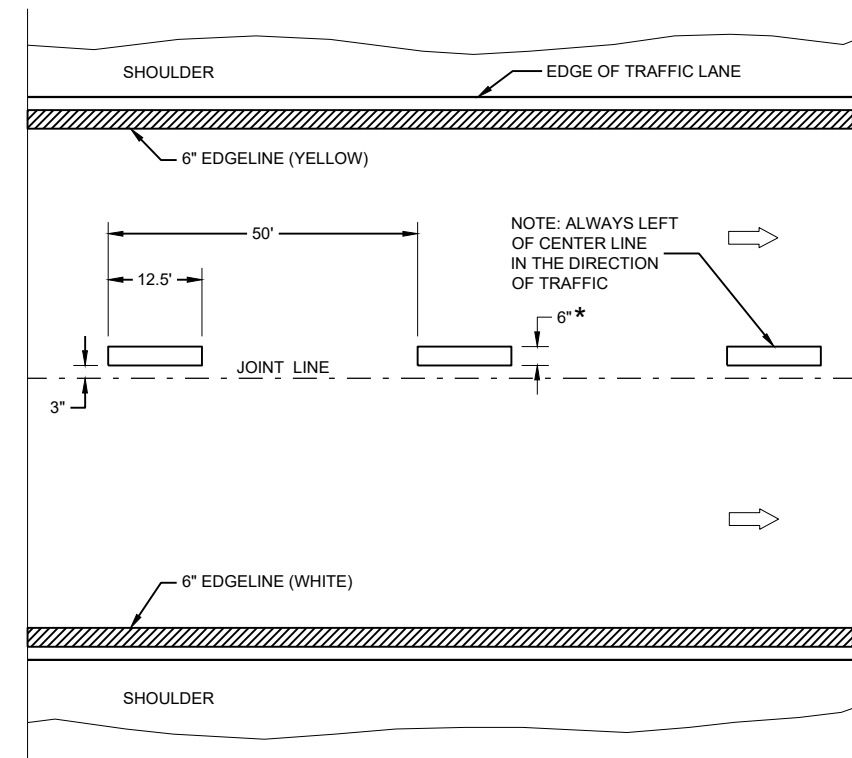
* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES



TWO WAY TRAFFIC



ONE WAY TRAFFIC



FREEWAYS AND EXPRESSWAYS

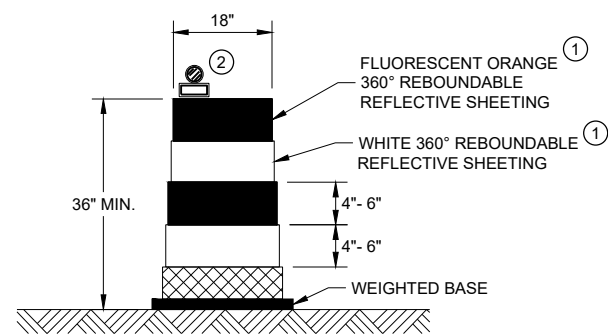
TEMPORARY PAVEMENT MARKING

TEMPORARY LONGITUDINAL PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

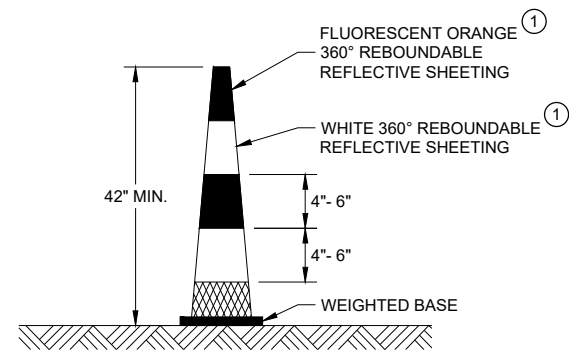
APPROVED
May 2023 /S/ Jeannie Silver
DATE STATEWIDE SIGNING AND MARKING ENGINEER

FHWA



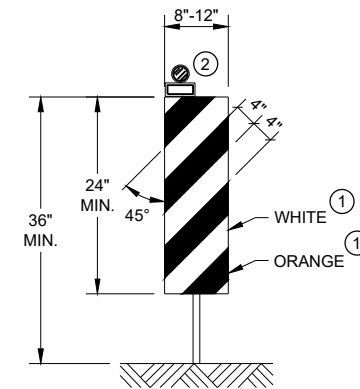
DRUM

BALLAST WIDTHS
RANGE FROM 24"-36"



42" CONE

DO NOT USE IN TAPERS
1/2 SPACING OF DRUMS
BALLAST WIDTHS
RANGE FROM 14"-20"

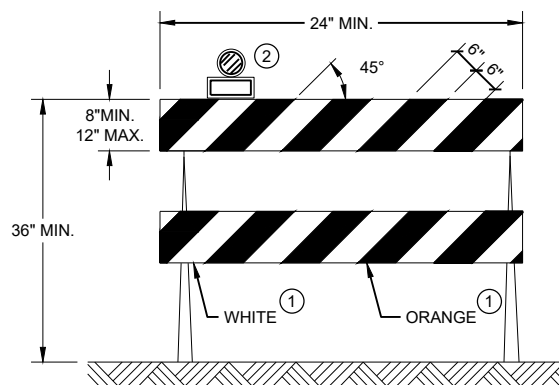


VERTICAL PANEL

THE STRIPES SHALL SLOPE DOWNWARD TO
THE TRAFFIC SIDE FOR CHANNELIZATION.

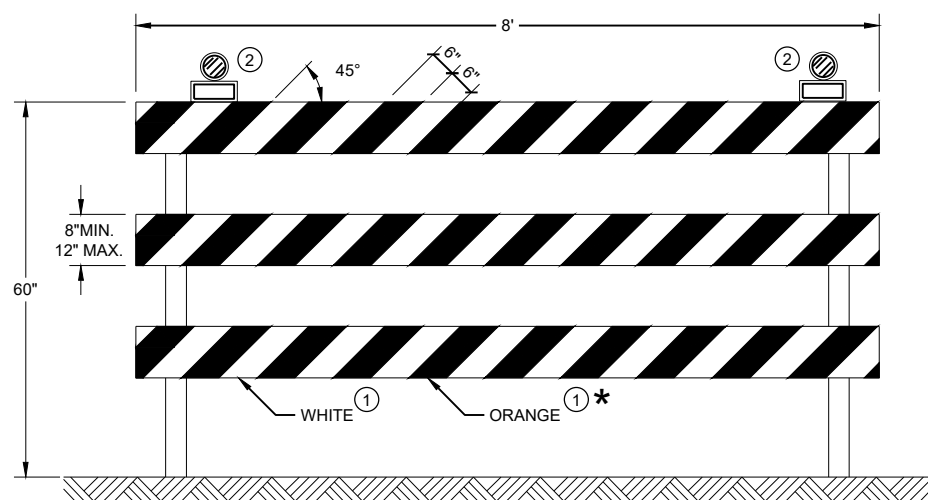
GENERAL NOTES

- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.



TYPE II BARRICADE

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES
MAY BE USED. ALL STRIPES SHALL SLOPE DOWNWARD
TO THE TRAFFIC SIDE FOR CHANNELIZATION.








TYPE III BARRICADE

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP
TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.

* IF USED FOR A PERMANENT APPLICATION USE RED SHEETING.

CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED November 2022 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

LEGEND

-  SIGN ON PORTABLE OR PERMANENT SUPPORT
-  TEMPORARY PORTABLE RUMBLE STRIP ARRAY
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS, DEVICES, AND LOCATION OF ALL FLAGGERS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGING

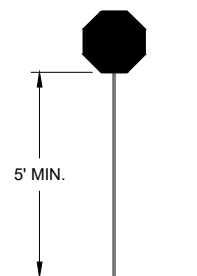
FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT REMOVE TEMPORARY PORTABLE RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.

- ① FOR MOVING WORK OPERATIONS, POST ADDITIONAL W20-7A FLAGGER SIGNS AT APPROXIMATELY 3,500' INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.
- ② SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

WHEN THE DISTANCE BETWEEN FLAGGERS EXCEEDS 2 MILES, A PILOT CAR IS REQUIRED. WHEN CURVES REDUCE SIGHT DISTANCE BELOW 400', A PILOT CAR IS REQUIRED.

TEMPORARY PORTABLE RUMBLE STRIPS

- UTILIZE TEMPORARY PORTABLE RUMBLE STRIPS ON ALL FLAGGING OPERATIONS.
- ③ EACH TEMPORARY PORTABLE RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS PLACED TRANSVERSE ACROSS THE LANE AT THE LOCATIONS SHOWN. WITHIN EACH ARRAY, SPACING BETWEEN RUMBLE STRIPS SHALL BE 15 FEET ON CENTER
- ONLY USE TEMPORARY PORTABLE RUMBLE STRIPS FROM THE APPROVED PRODUCTS LIST.
- INSTALL TEMPORARY RUMBLE STRIPS PER MANUFACTURER'S RECOMMENDATIONS.
- PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.
- DO NOT INSTALL TEMPORARY PORTABLE RUMBLE STRIPS ON GRAVEL, MILLED SURFACES, OR ASPHALT THAT HAS BEEN PAVED LESS THAN 12 HOURS.



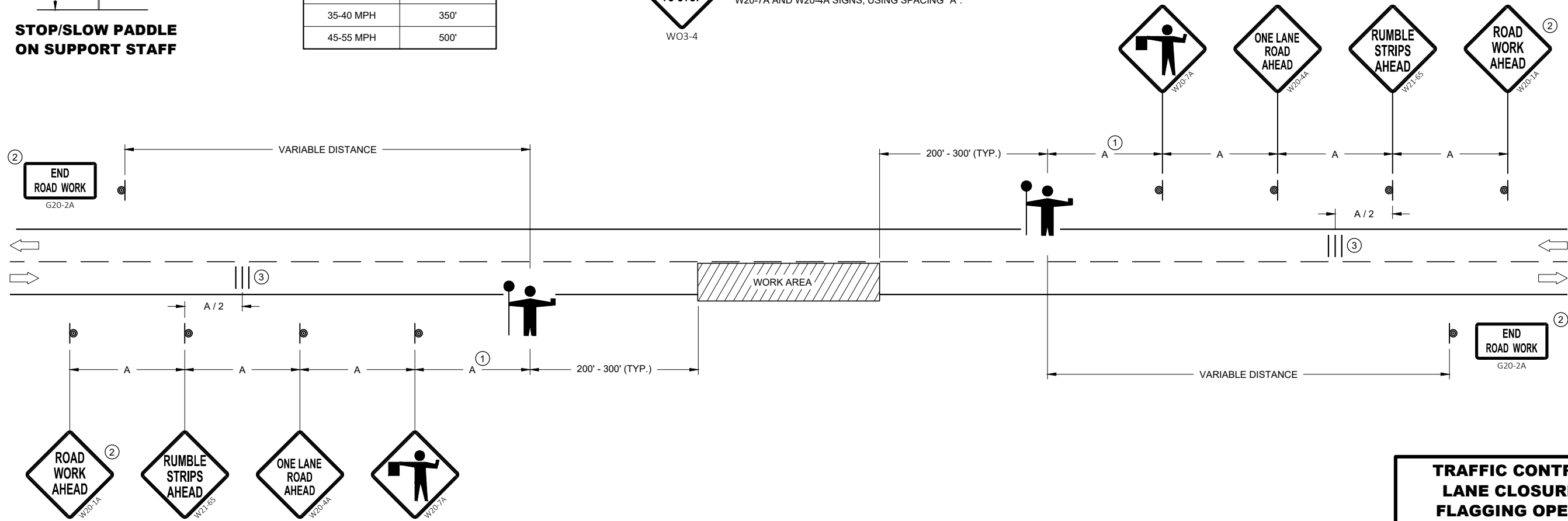
STOP/SLOW PADDLE ON SUPPORT STAFF

SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE

SPEED LIMIT	SPACING "A"
25-30 MPH	200'
35-40 MPH	350'
45-55 MPH	500'



USE OF W03-4 SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7A AND W20-4A SIGNS, USING SPACING "A".



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

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SDD 15C12 - 09a

SDD 15C12 - 09a

TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

GENERAL NOTES

-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL CONE 42-INCH
-  TRAFFIC CONTROL DRUM
-  TEMPORARY PORTABLE RUMBLE STRIP ARRAY
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  **AFAD** AUTOMATED FLAGGER ASSISTANCE DEVICE (AFAD)

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS, DEVICES, AND LOCATION OF ALL FLAGGERS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGING

IF THE AUTOMATED FLAGGER ASSISTANCE DEVICE (AFAD) STOPS WORKING, FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT REMOVE TEMPORARY PORTABLE RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.

WHEN THE DISTANCE BETWEEN FLAGGERS EXCEEDS 2 MILES, A PILOT CAR IS REQUIRED. WHEN CURVES REDUCE SIGHT DISTANCE BELOW 400', A PILOT CAR IS REQUIRED.

- ① SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.
- ② IF FLAGGERS ARE PHYSICALLY NEEDED TO FLAG, REPLACE WO3-4 SIGNS WITH W20-7A SIGNS.

TEMPORARY PORTABLE RUMBLE STRIPS

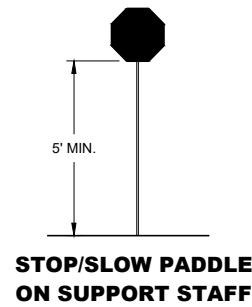
UTILIZE TEMPORARY PORTABLE RUMBLE STRIPS ON ALL FLAGGING OPERATIONS.

ONLY USE TEMPORARY PORTABLE RUMBLE STRIPS FROM THE APPROVED PRODUCTS LIST.

PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.

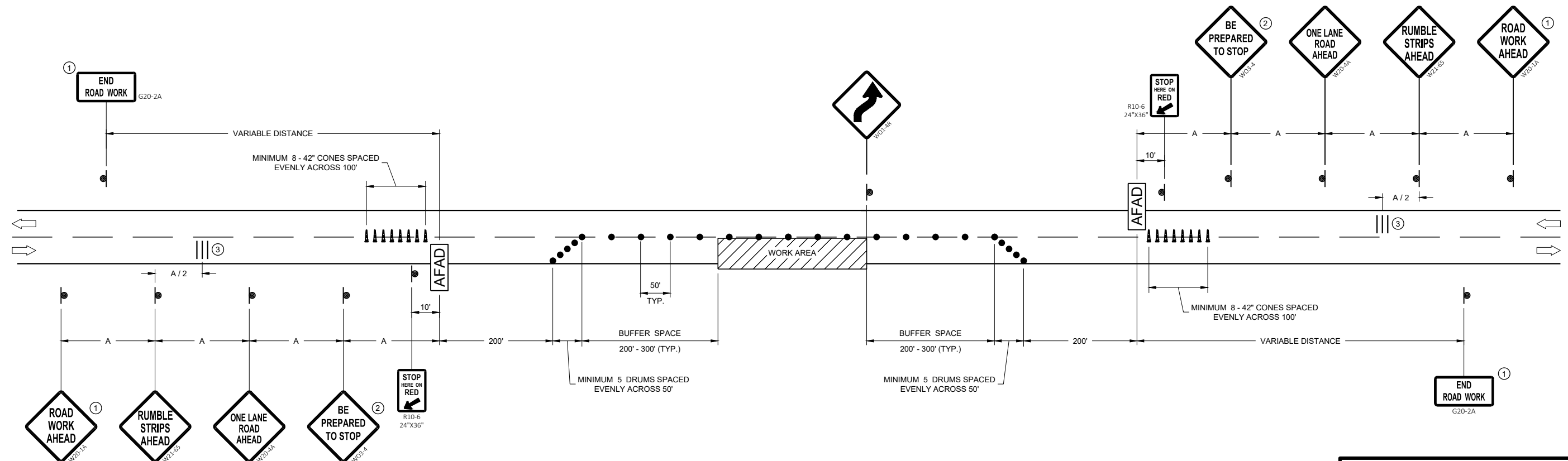
DO NOT INSTALL TEMPORARY PORTABLE RUMBLE STRIPS ON GRAVEL, MILLED SURFACES, OR ASPHALT THAT HAS BEEN PAVED LESS THAN 12 HOURS.

③ EACH TEMPORARY PORTABLE RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS PLACED TRANSVERSELY AT THE LOCATIONS SHOWN. WITHIN EACH ARRAY, SPACING BETWEEN RUMBLE STRIPS SHALL BE 15 FEET ON CENTER.



SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE

SPEED LIMIT	SPACING "A"
25-30 MPH	200'
35-40 MPH	350'
45-55 MPH	500'



TRAFFIC CONTROL, LANE CLOSURE WITH AUTOMATED FLAGGER ASSISTANCE DEVICE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2022 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA





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SDD 15C12 - 09b

SDD 15C12 - 09b

LEGEND

- V1 LEAD VEHICLE
- V2 MARKING VEHICLE
- V3 SHADOW VEHICLE
-  TRUCK MOUNTED ATTENUATOR (TMA)
-  SIGN ON TEMPORARY SUPPORT
-  DIRECTION OF TRAFFIC
-  FLASHING ARROW PANEL (CAUTION)

GENERAL NOTES

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

ALL VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL OPERATING IN CAUTION MODE. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.

DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE, PAINT DRYING TIME, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL AND HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.

THE WORK AND SHADOW VEHICLES SHOULD PULL OVER PERIODICALLY TO ALLOW TRAFFIC TO PASS.

WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR LAY STATIONARY SIGNS AND SUPPORTS FLAT ON THE GRADE WITH UPRIGHTS ORIENTED PARALLEL TO AND DOWNSTREAM FROM TRAFFIC.

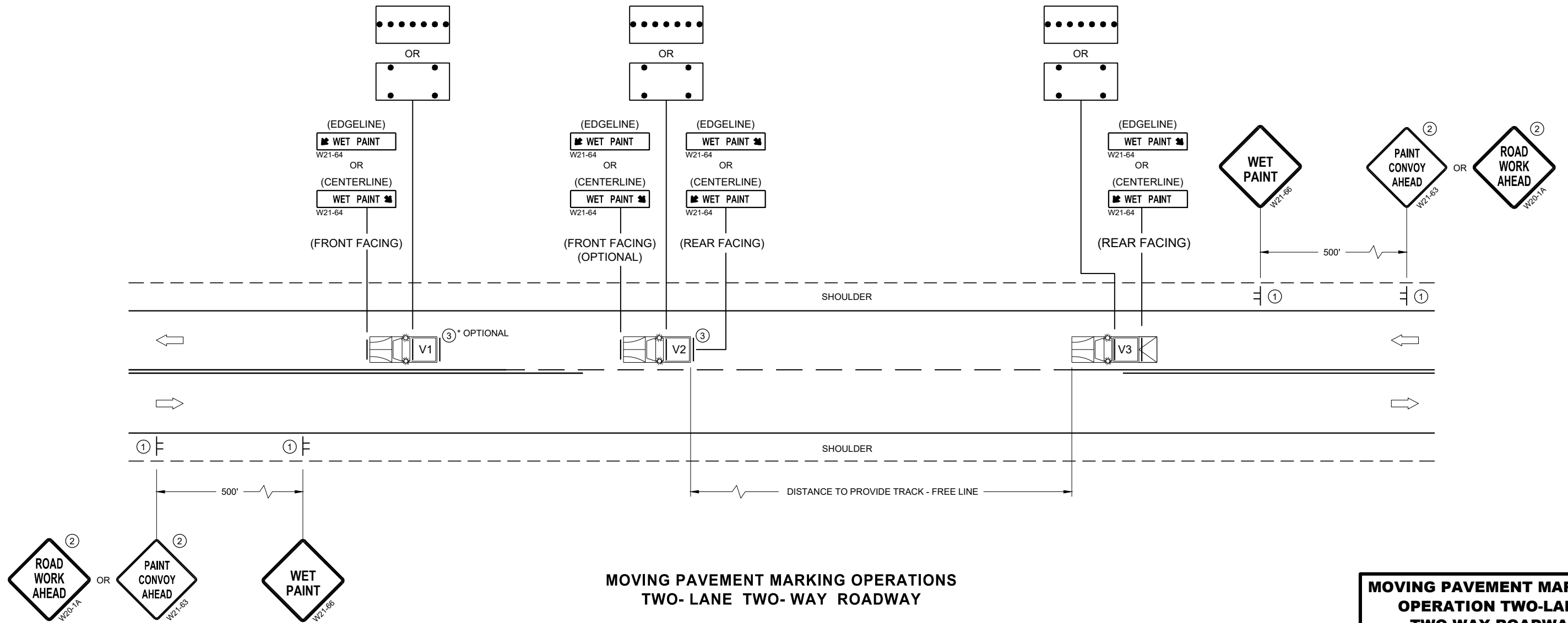
CONES SHOULD BE USED BETWEEN THE MARKING AND SHADOW VEHICLE AT 100 FOOT SPACING. CONES MAY BE OMITTED ON PAINTED LINE IF APPROVED BY THE ENGINEER. CONSIDER PAVEMENT MARKING DRY OR CURE TIMES AND TRAFFIC VOLUME.

CONES SHALL BE A MINIMUM OF 28" FOR WET PAVEMENT MARKING.

- ① SIGNS SHALL BE REPEATED APPROXIMATELY EVERY THREE MILES.
- ② IF CONSTRUCTION WORK ZONE SIGNS ARE IN PLACE, W20-1A OR W21-63 ARE NOT REQUIRED.
- ③ V1 AND V2 CAN BE SWITCHED SO THAT THE MARKER IS THE LEAD VEHICLE.

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**MOVING PAVEMENT MARKING OPERATIONS
TWO-LANE TWO-WAY ROADWAY**

SDD 15C19-08a

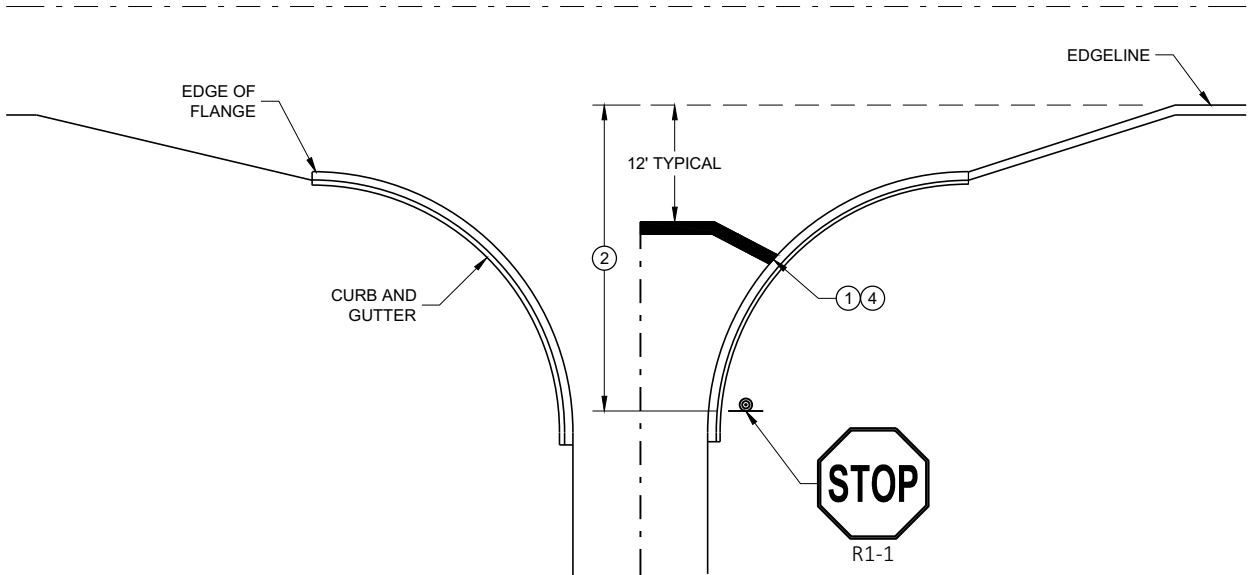
SDD 15C19-08a

MOVING PAVEMENT MARKING OPERATION TWO-LANE TWO-WAY ROADWAY	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED February 2023 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

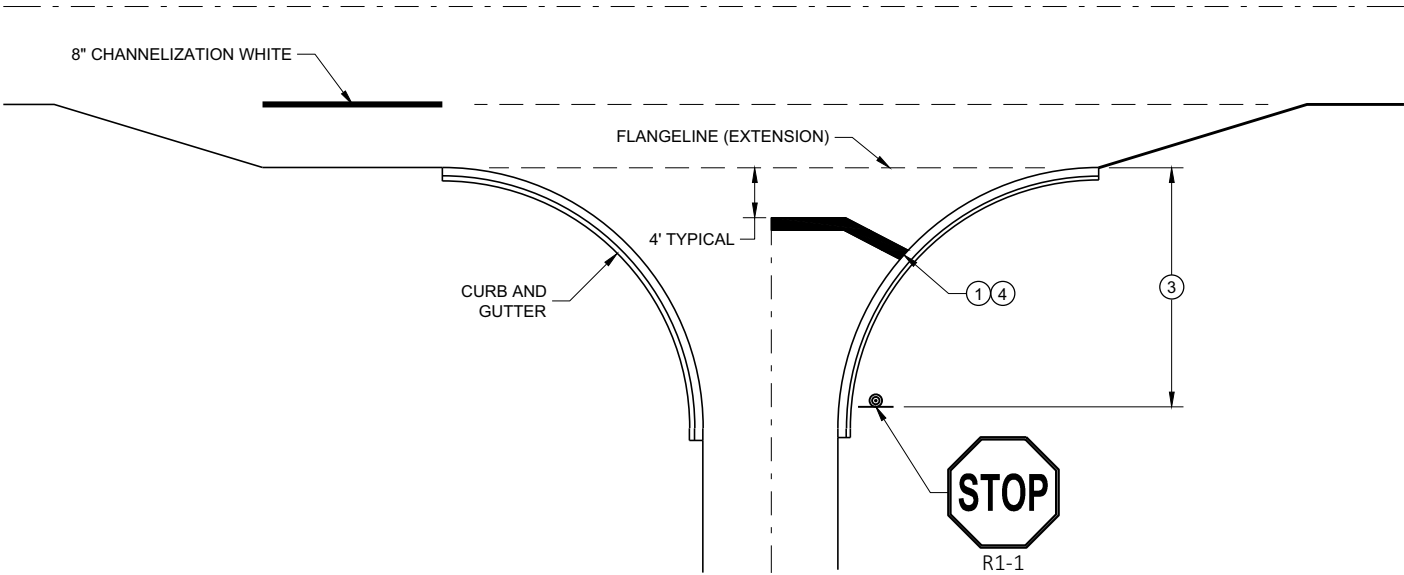
GENERAL NOTES

STOP SIGN SHALL BE PLACED A MINIMUM OF 6 FEET TO A MAXIMUM OF 50 FEET FROM THE EDGELINE LOCATION.

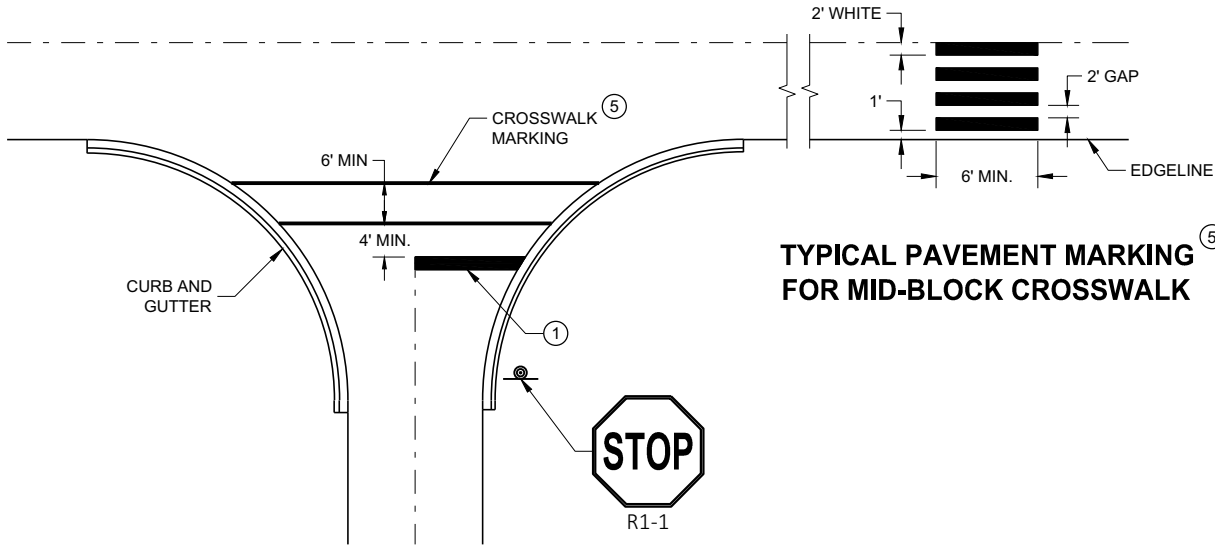
- ① 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE REGION MARKING ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
- ② NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGELINE.
- ③ NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGELINE EXTENSION.
- ④ MOVE CLOSER TO THE EDGE OF TRAVEL LINE AS NEEDED FOR VISIBILITY AND SIGHT LINES (NO CLOSER THAN 4 FEET).
- ⑤ LADDER BAR CROSSWALKS SHOULD ONLY BE USED FOR MID BLOCK CROSSINGS. USE 2 - 6" TRANSVERSE LINES INSTEAD.



TYPICAL STOP LINE PAVEMENT MARKING WITH CURB AND GUTTER

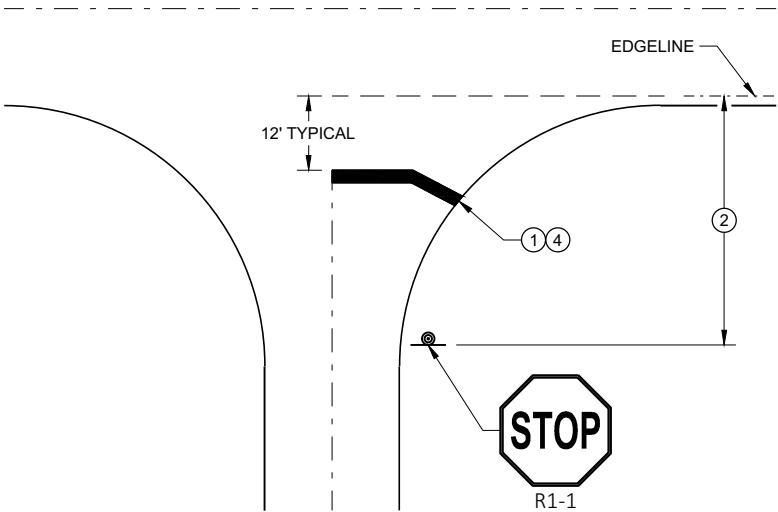


TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH RIGHT TURN LANE



TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH CROSSWALK MARKING

TYPICAL PAVEMENT MARKING FOR MID-BLOCK CROSSWALK



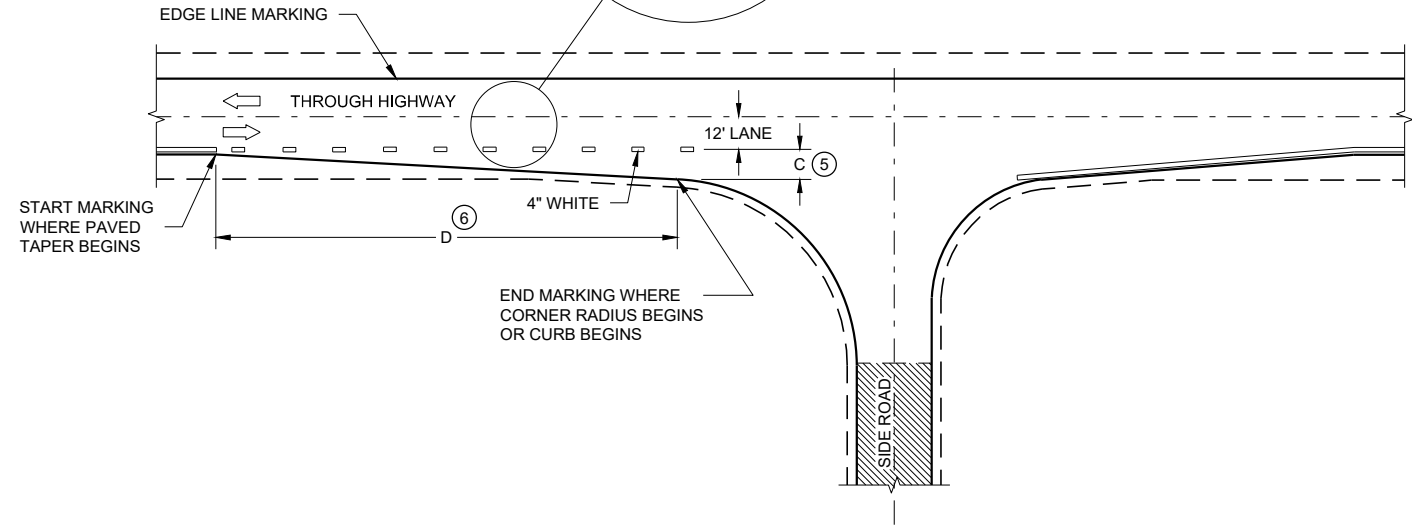
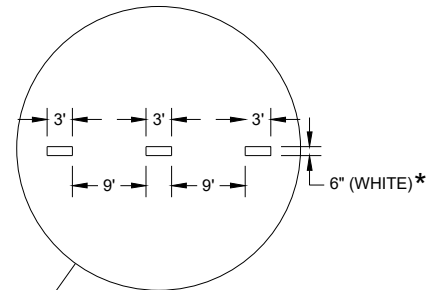
TYPICAL STOP LINE PAVEMENT MARKING WITHOUT CURB AND GUTTER

STOP LINE AND CROSSWALK PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2019 /S/ Matthew Rauch
DATE STATE SIGNING AND MARKING ENGINEER

FHWA



MINOR INTERSECTION

* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES

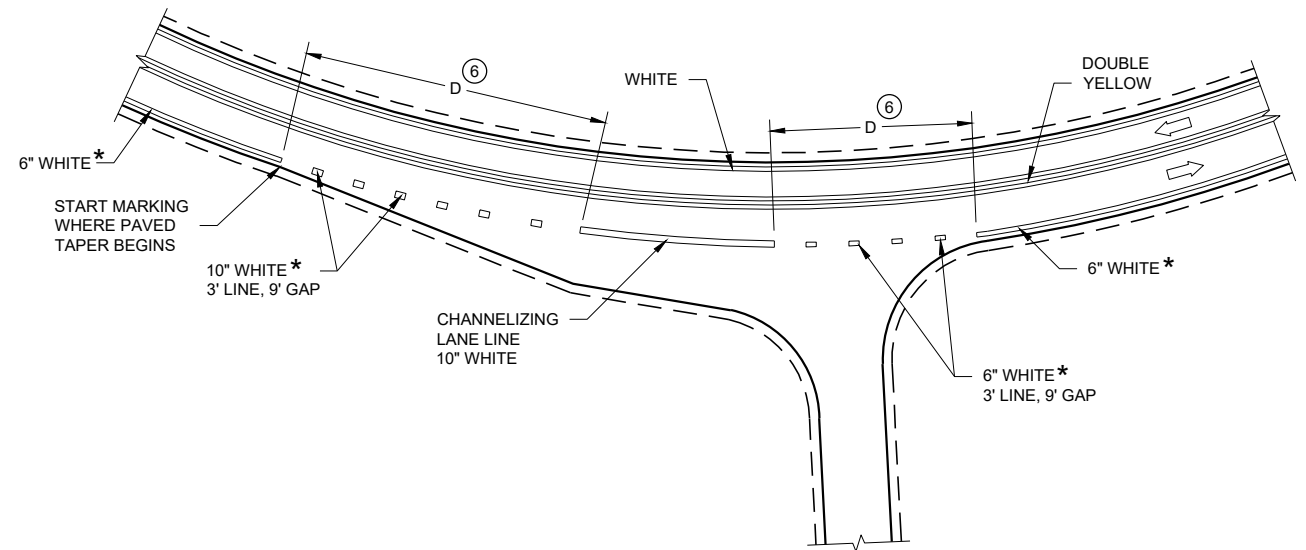
GENERAL NOTES

OMIT EDGE LINES THROUGH INTERSECTIONS. CONTINUE EDGE LINES THROUGH DRIVEWAYS.

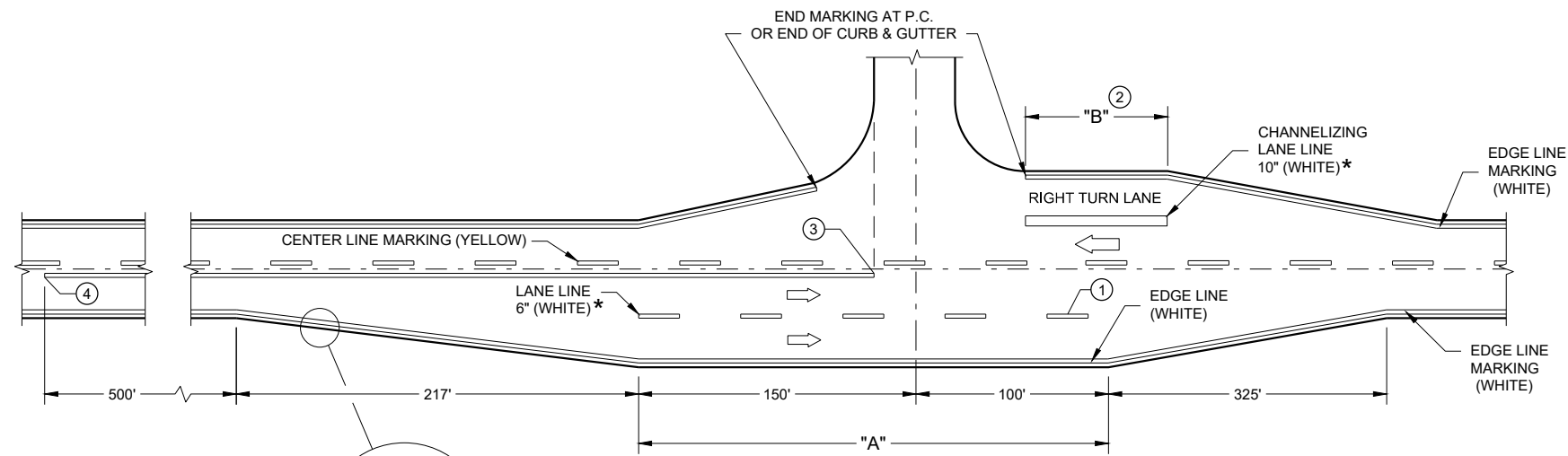
- ① WHEN DISTANCE "A" IS LESS THAN 250 FEET, OMIT LANE LINE.
- ② WHEN DISTANCE "B" IS LESS THAN 100 FEET, OMIT CHANNELIZING LANE LINE.
- ③ BARRIER LINE ENDS AT SIDE ROAD PAVEMENT / SURFACE EDGE EXTENSION.
- ④ BARRIER LINE STARTS 500 FEET PRIOR TO THE BYPASS TAPER.
- ⑤ WHEN DISTANCE "C" IS LESS THAN 4 FEET, OMIT DOTTED EXTENSION.
- ⑥ WHEN DISTANCE "D" IS LESS THAN 50 FEET, OMIT DOTTED EXTENSION.

LEGEND

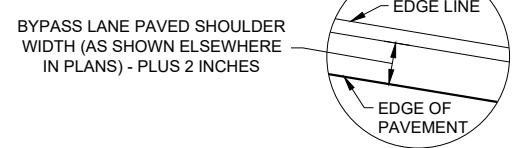
➔ DIRECTION OF TRAVEL



INTERSECTION ON OUTSIDE OF CURVE







**MAJOR INTERSECTIONS
(INTERSECTION WITH FULL RIGHT TURN LANE OR BYPASS LANE)**



BYPASS LANE PAVED SHOULDER WIDTH (AS SHOWN ELSEWHERE IN PLANS) - PLUS 2 INCHES

PAVEMENT MARKING (INTERSECTIONS)
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  DIRECTION OF TRAFFIC
-  WORK ZONE

GENERAL NOTES

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" SIGNS MAY BE USED IF APPROVED BY THE REGIONAL TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

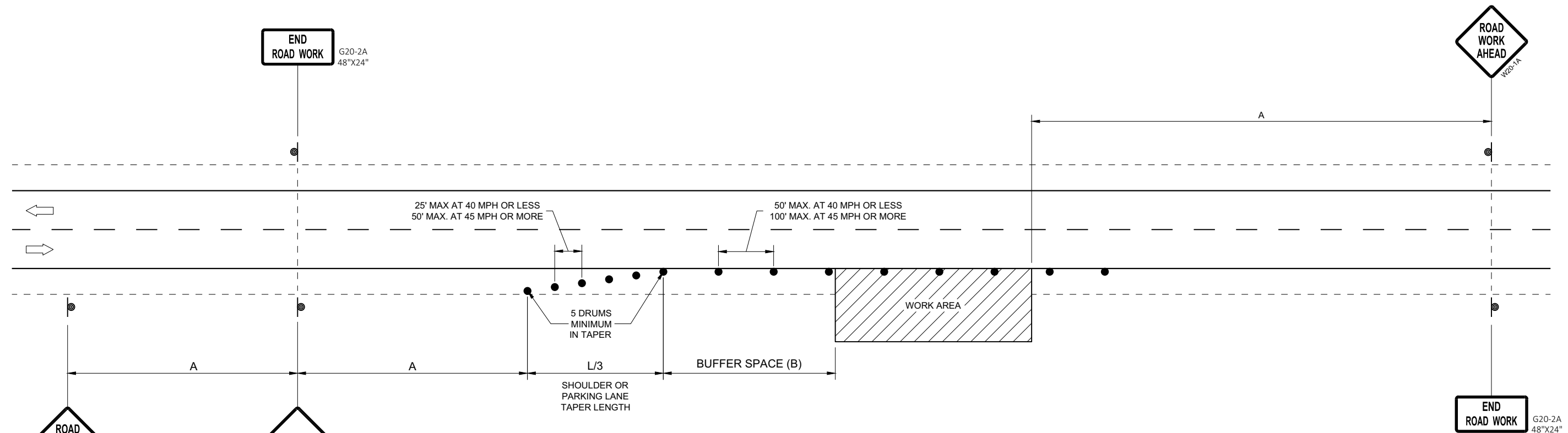
CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

W20-1A AND G20-2A SIGNS ARE NOT REQUIRED IF THE WORK AREA IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT. G20-2A SIGNS MAY ALSO BE OMITTED IF DURATION OF WORK IS LESS THAN 7 CONTINUOUS DAYS AND NIGHTS.

6

6



OR
IF TRAFFIC CONTROL DEVICES
ENCROACH ONTO TRAVELED WAY, USE

POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET	SHOULDER TAPER L / 3 W, LATERAL OFFSET (FT)						BUFFER SPACE (B) FEET
		3	4	5	6	7	8	
25	200'	10	14	17	21	24	28	55
30	200'	15	20	25	30	35	40	85
35	350'	20	27	34	40	47	54	120
40	350'	26	35	44	53	62	70	170
45	500'	45	59	74	89	104	119	220
50	500'	50	66	83	99	116	132	280
55	500'	54	73	91	109	127	145	335'

**TRAFFIC CONTROL, WORK ON
SHOULDER OR PARKING LANE,
UNDIVIDED ROADWAY**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION








APPROVED
May 2020 /S/ Andrew Heidtke
DATE STATEWIDE WORK ZONE TRAFFIC
SAFETY ENGINEER

FHWA

SDD 15D28 - 04

SDD 15D28 - 04

LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  SIGN ON PERMANENT SUPPORT
-  TYPE III BARRICADE
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  TYPE "A" WARNING LIGHT (FLASHING)
-  DIRECTION OF TRAFFIC
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

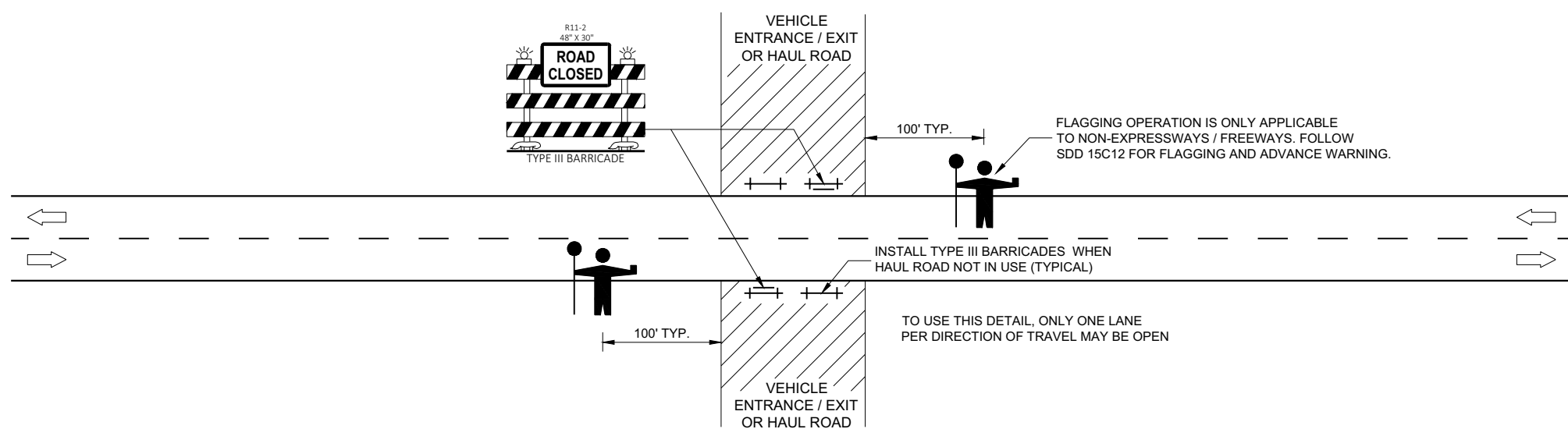
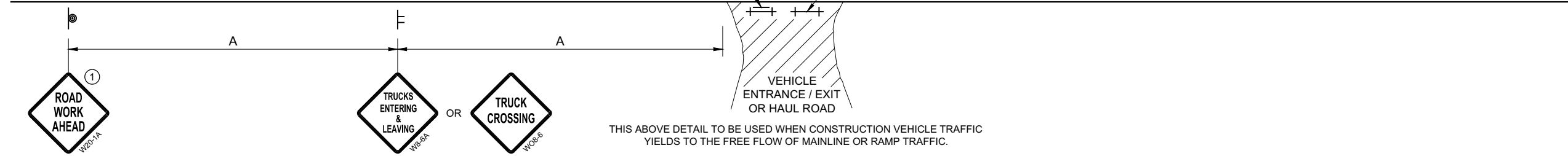
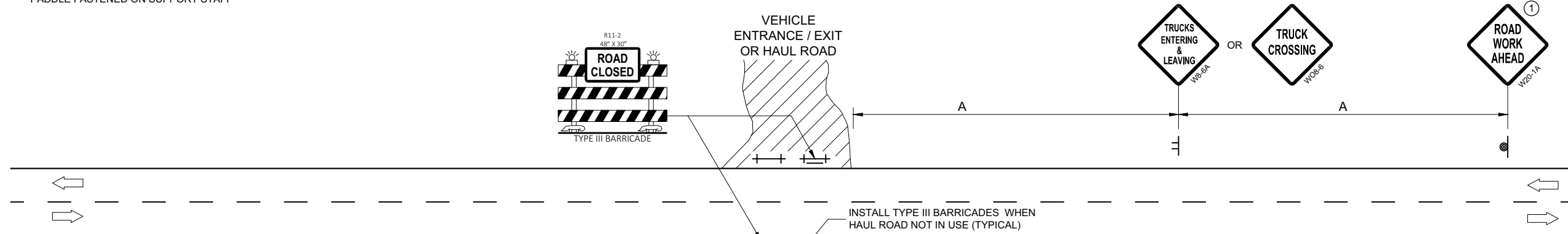
POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET
0-30	200'
35-40	350'
45-55	500'

GENERAL NOTES

- ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED.
- "WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.
- THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.
- WARNING SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.
- WHEN ACTIVITY REFLECTED BY THE SIGN IS NOT CURRENTLY TAKING PLACE, THE HIGHWAY SHALL BE RESTORED TO NORMAL CONDITION AND THE SIGNS SHALL BE REMOVED, COVERED OR TURNED AWAY FROM TRAFFIC.
- WHEN A SIDE ROAD OR RAMP INTERSECTS WITHIN THE ADVANCE SIGNING AREA, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND / OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.
- PLACE SIGNS ON BOTH SIDES IF USED ON DIVIDED HIGHWAY.
- ① THESE SIGNS ARE TO BE USED ONLY WHEN VEHICLE ENTRANCE / EXIT CONDITIONS ARE SEPARATED BY MORE THAN TWO MILES FROM PREVIOUS WORK AREA OR SIGNING OR AS DIRECTED BY THE ENGINEER.

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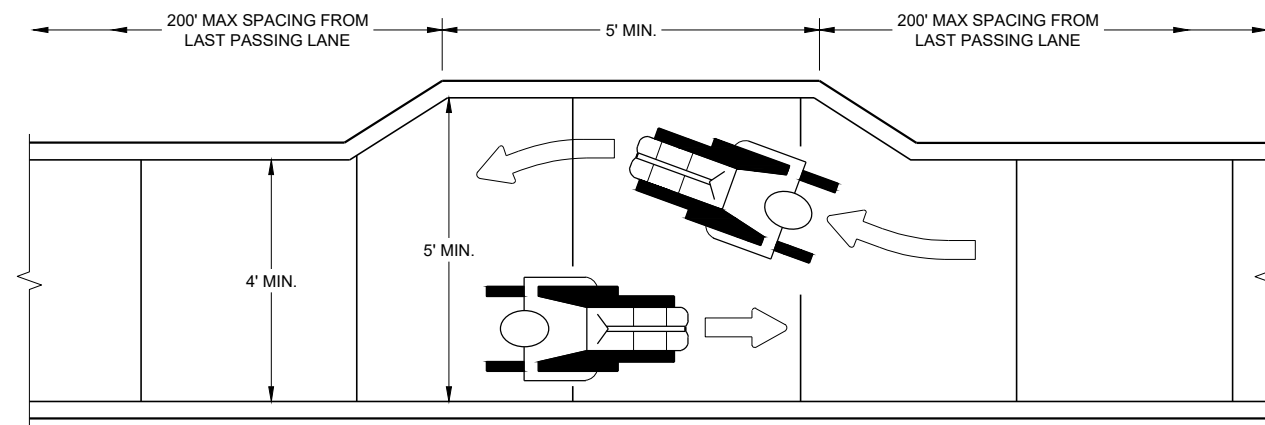
THIS DETAIL TO BE USED WHEN CONSTRUCTION WORK INCLUDING TRUCKING ACTIVITY REQUIRES MAINLINE TRAFFIC TO BE TEMPORARILY STOPPED IN ONE OR BOTH DIRECTIONS. DELAY TO HIGHWAY TRAFFIC SHALL BE MINIMIZED.

FLAGGING OPERATION IS ONLY APPLICABLE TO NON-EXPRESSWAYS / FREEWAYS. FOLLOW SDD 15C12 FOR FLAGGING AND ADVANCE WARNING.

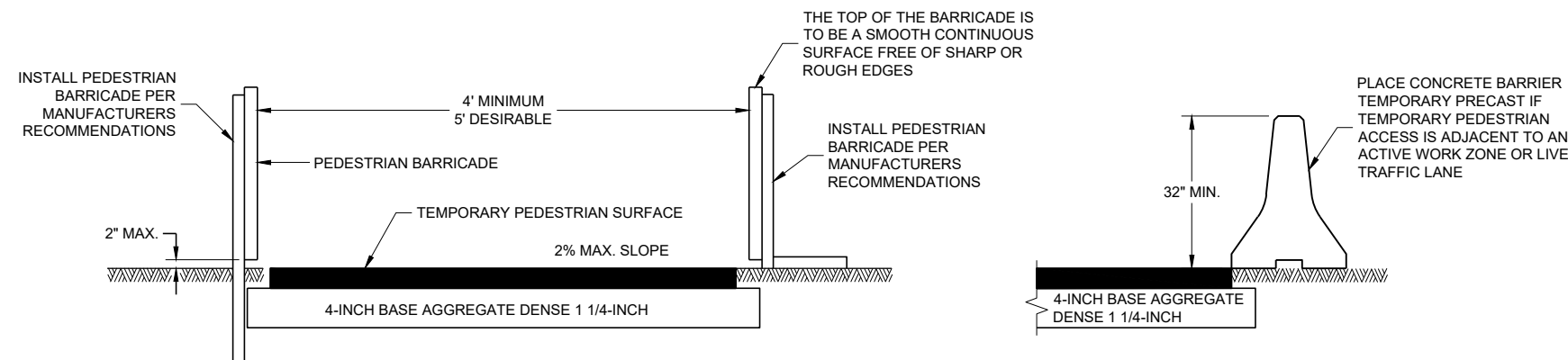
SDD 15D29 - 06

SDD 15D29 - 06

TRAFFIC CONTROL, VEHICLE ENTRANCE/EXIT OR HAUL ROAD	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2020 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	



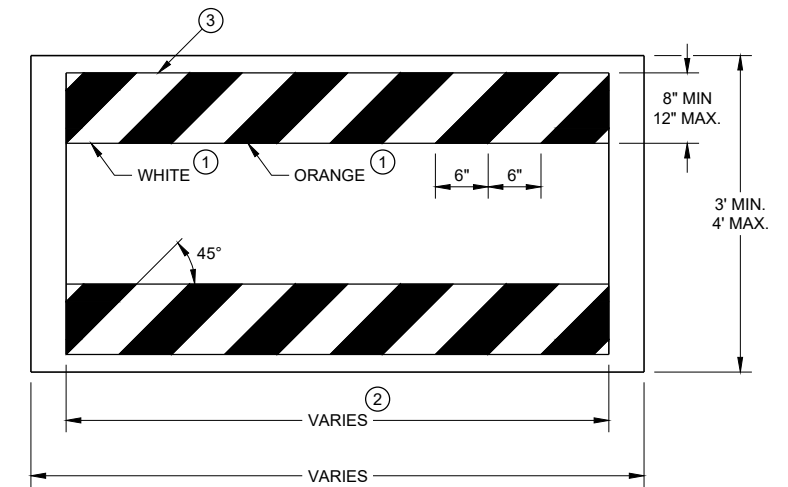
NARROW SIDEWALK PASSING DETAIL



TEMPORARY PEDESTRIAN ACCESS

GENERAL NOTES

- BARRICADE DEVICE SELECTED FROM APPROVED PRODUCT LIST
- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② SHEETING REQUIRED ON MORE THAN 50% OF BARRICADE WIDTH.
- ③ PLACE SHEETING ON BOTH SIDES OF THE BARRICADE.
- * USE THIS DETAIL FOR SHEETING PLACEMENT REFERENCE.

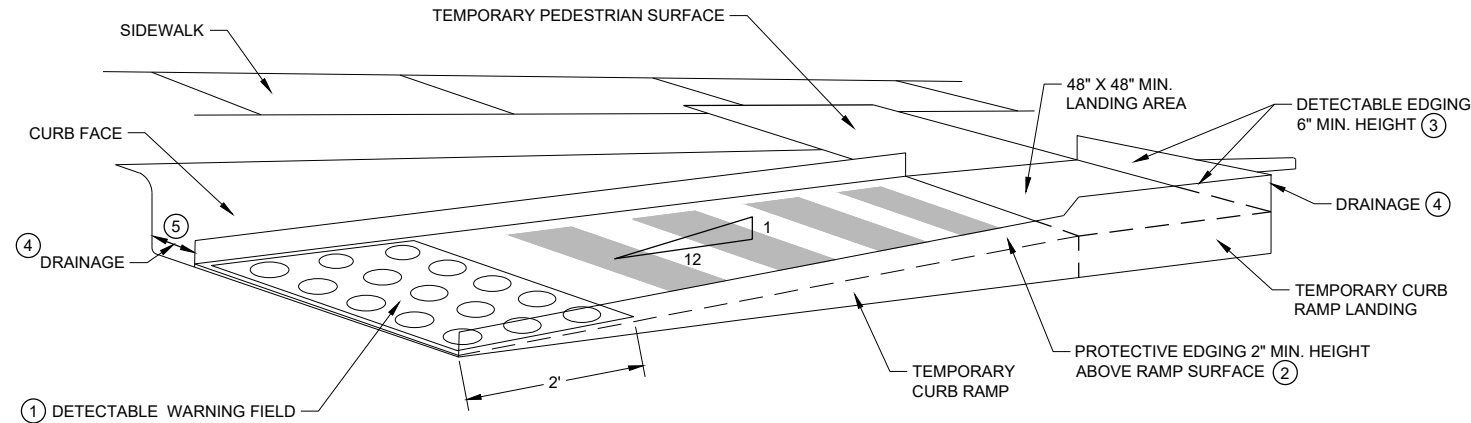


TEMPORARY PEDESTRIAN BARRICADE*

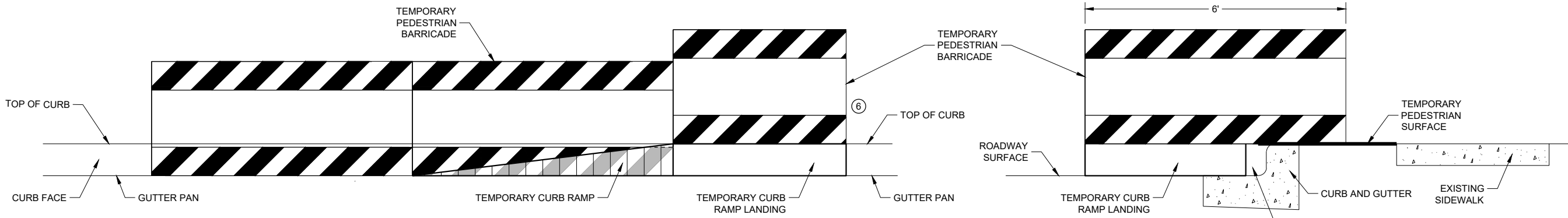
GENERAL NOTES

CURB RAMPS SHALL BE 48" MIN. WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE.
 CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.
 CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.
 LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.
 CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES MAY BE VERTICAL UP TO 1/4" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".

- ① INSTALL CONTRASTING TEMPORARY DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS, AS SHOWN IN THE PLANS.
- ② PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- ③ DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- ④ DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ⑤ ENSURE CURB RAMP IS OUT OF THE GUTTER PAN.
- ⑥ IF ONLY PART OF THE END PANEL OF TEMPORARY PEDESTRIAN BARRICADE PANEL IS NEEDED, EXTEND EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL HERE.



PERSPECTIVE VIEW

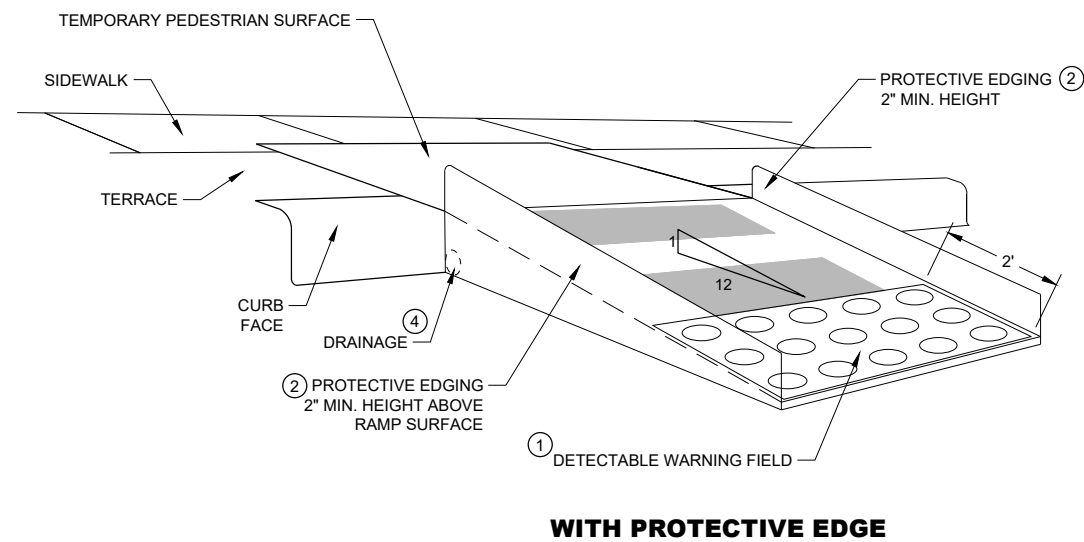
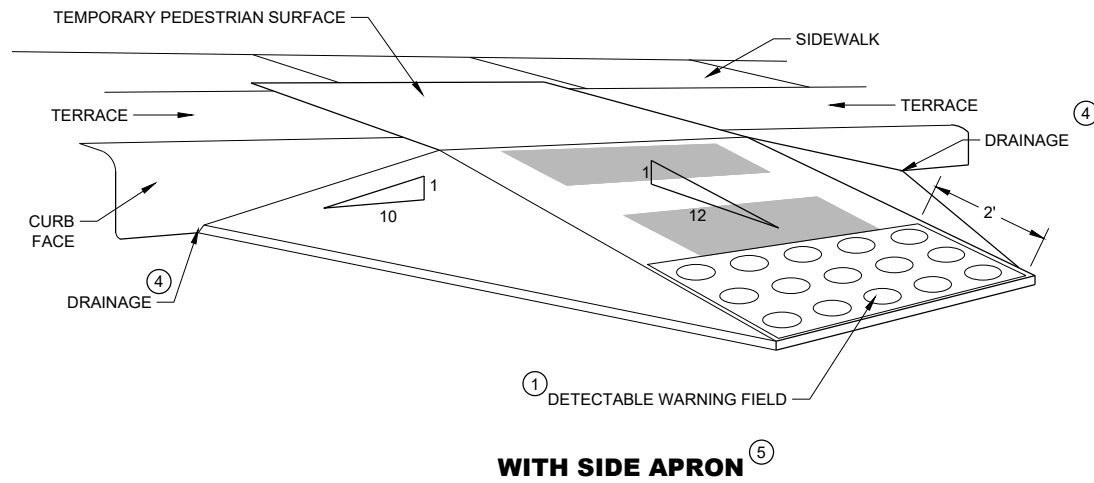


FRONT VIEW

SIDE VIEW

TEMPORARY CURB RAMP PARALLEL TO CURB

<p>TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION</p>
<p>STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION</p>



TEMPORARY CURB RAMP PERPENDICULAR TO CURB

GENERAL NOTES

CURB RAMPS SHALL BE 48" MINIMUM WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE.

ALTERNATE SIDEWALK WORK BETWEEN LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.

CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.







CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.

LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.

CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES MAY BE VERTICAL UP TO 1/4" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".

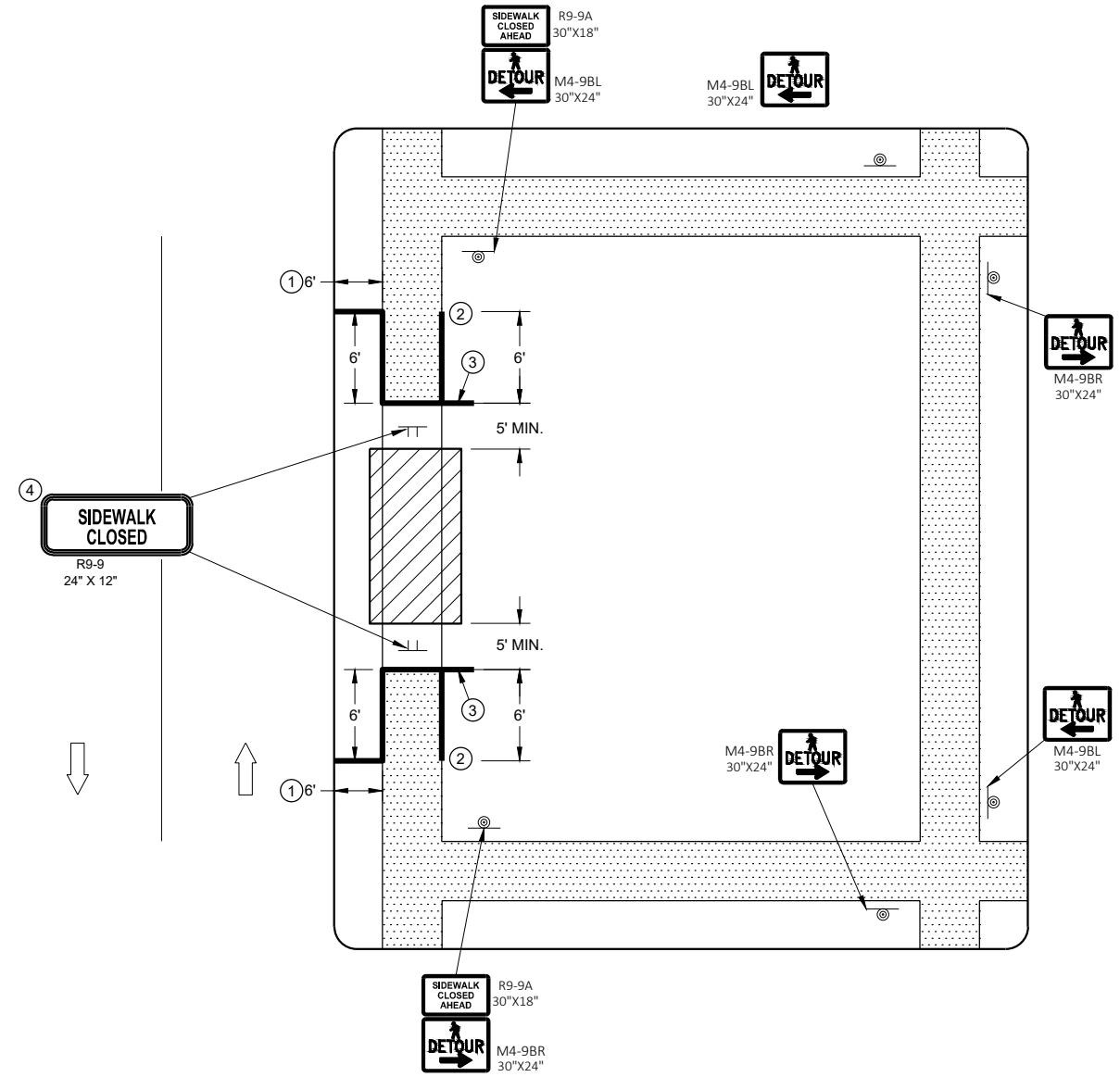
- ① INSTALL CONTRASTING TEMPORARY DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS, AS SHOWN IN THE PLANS
- ② PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- ③ DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- ④ DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ⑤ CAN ONLY BE USED FOR RAMPS WITH 6" OR LESS OF VERTICAL CHANGE.

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  SIGN ON TEMPORARY SUPPORT
-  UNDER PEDESTRIAN TRAFFIC
-  WORK AREA
-  TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC

GENERAL NOTES

- SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.
- WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.
- SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.
- PLACE TEMPORARY PEDESTRIAN BARRICADE TO FIT FIELD CONDITIONS, AVOIDING CONFLICTS WITH DRIVEWAYS AND OTHER EXISTING FEATURES.
- ① IF TERRACE IS LESS THAN 6 FEET WIDE, OMIT TEMPORARY PEDESTRIAN BARRICADE FROM THE SIDEWALK TO THE CURB.
 - ② PLACE BARRICADE CLOSURE SO THAT THE TEMPORARY PEDESTRIAN BARRICADE END IS AT THE LAST OPEN SIDEWALK ACCESS TO RESIDENCES OR BUSINESSES BEFORE THE SIDEWALK CLOSURE.
 - ③ IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
 - ④ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.



SIDEWALK DETOUR, SIDEWALK ONLY ON ONE SIDE

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION








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SDD 15D30 - 09f

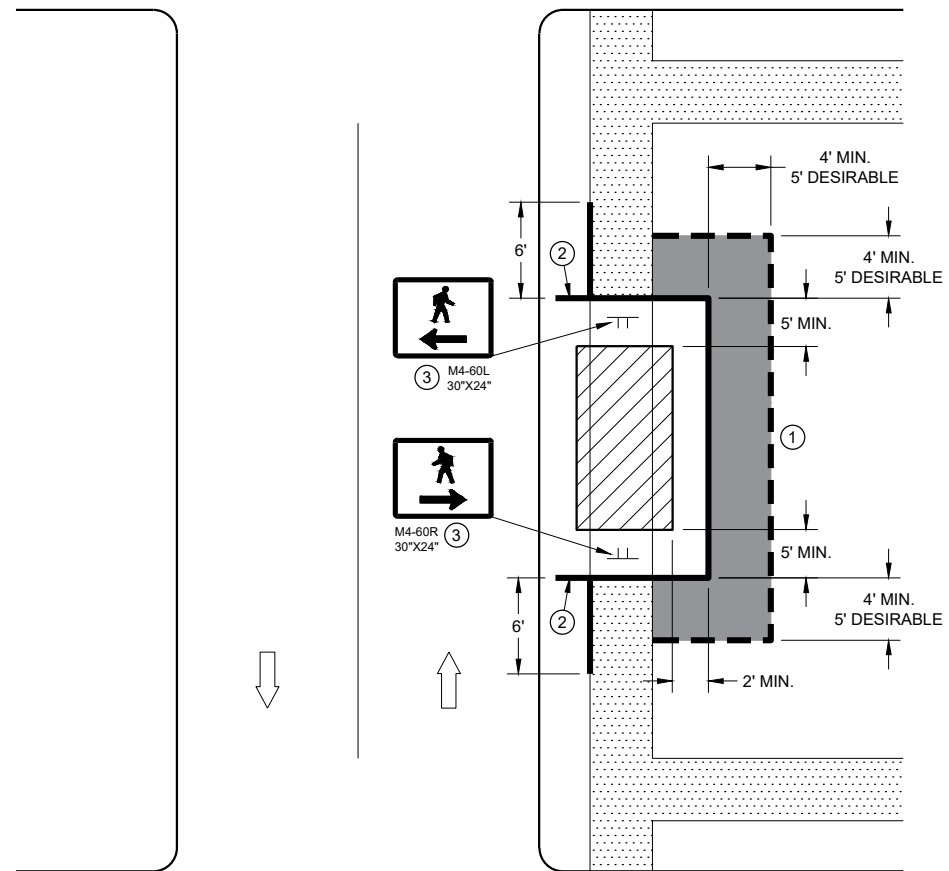
SDD 15D30 - 09f

LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  WORK AREA
-  UNDER PEDESTRIAN TRAFFIC
-  TEMPORARY PEDESTRIAN SURFACE
-  TEMPORARY PEDESTRIAN BARRICADE
-  OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC










GENERAL NOTES

- TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.
- SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.
- WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.
- SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.
- ① USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
- ② IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
- ③ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.



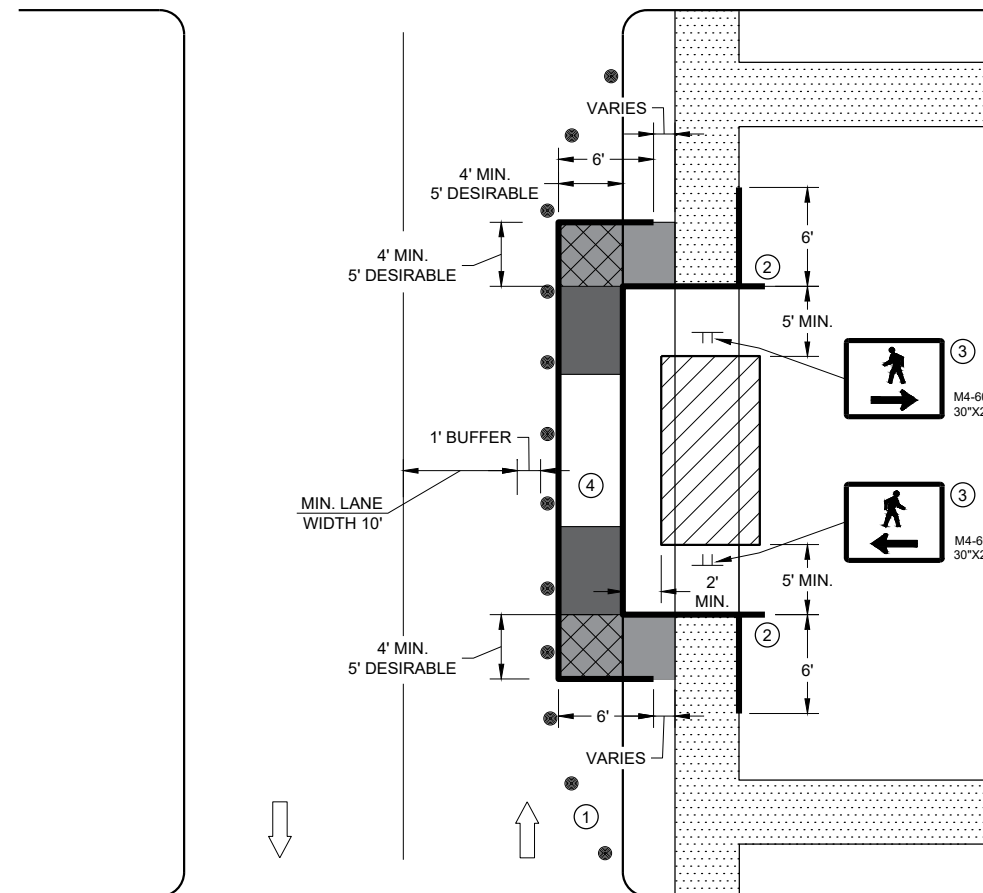
**SIDEWALK DIVERSION
SINGLE SIDE**

LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL DRUM
-  WORK AREA
-  UNDER PEDESTRIAN TRAFFIC
-  TEMPORARY CURB RAMP
-  TEMPORARY PEDESTRIAN SURFACE "A"
-  TEMPORARY PEDESTRIAN SURFACE "B"
-  TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC

GENERAL NOTES

- TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.
- SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.
- WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.
- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND BUFFER SPACE REQUIRED.
 - ② PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL PAST THE SIDEWALK ON THE SIDE AWAY FROM THE ROAD.
 - ③ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.
 - ④ USE EXISTING PAVEMENT SURFACE. IF EXISTING PAVEMENT SURFACE HAS BEEN REMOVED, USE A TEMPORARY PEDESTRIAN SURFACE.



SIDEWALK DIVERSION, SINGLE SIDE

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

IF PEDESTRIAN PUSH BUTTONS ARE PRESENT ON THE EXISTING FACILITY, ENSURE THEY ARE MAINTAINED/ACCESSIBLE FOR PEDESTRIAN USE THROUGHOUT THE TEMPORARY PEDESTRIAN ACCOMMODATIONS.











SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

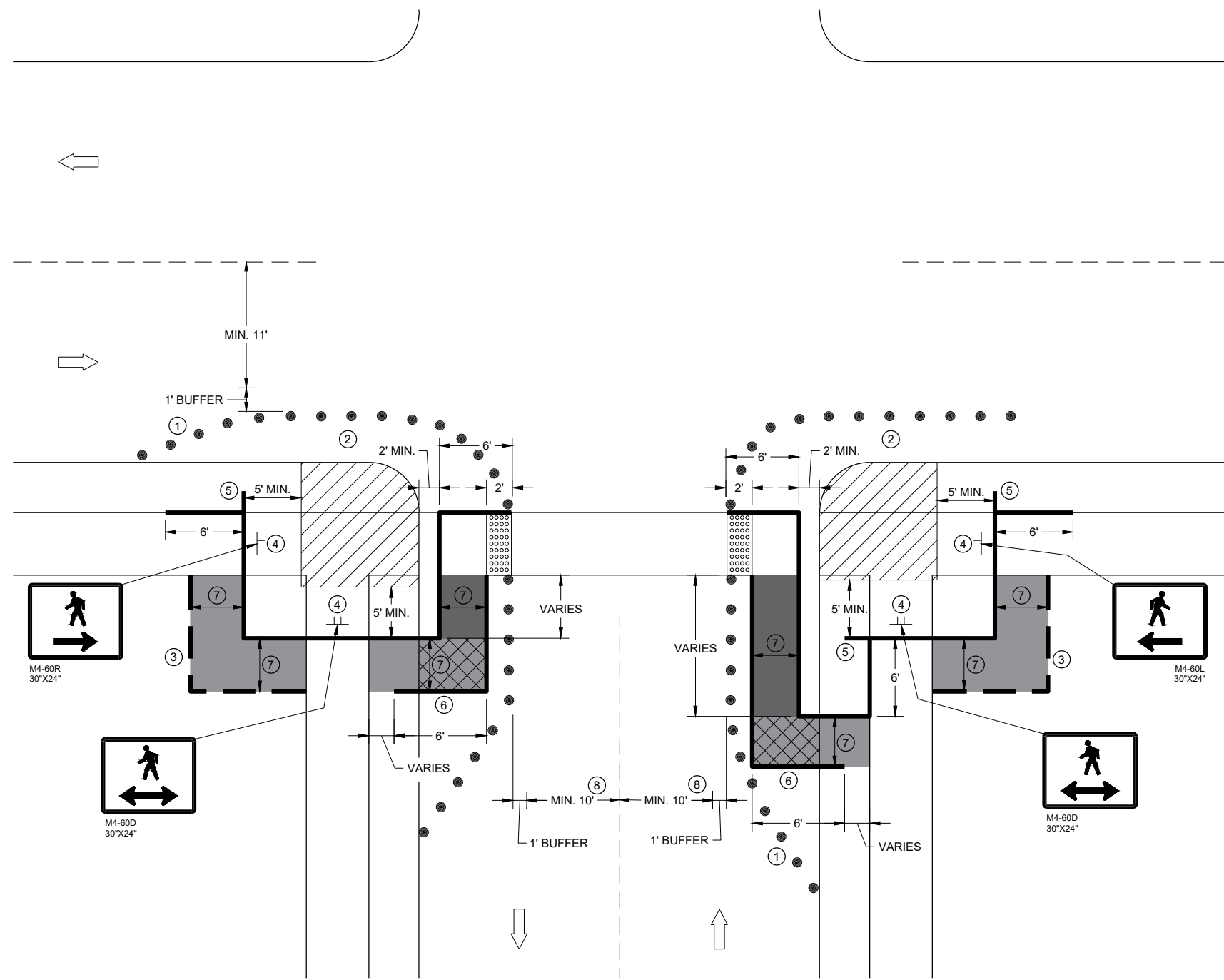
TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG

WHEN TEMPORARY PEDESTRIAN BARRICADE RUNS PARALLEL ALONG THE SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND PROPER BUFFER SPACE REQUIRED.
- ② PROVIDE ADEQUATE SPACE FOR CONTRACTOR OPERATIONS
- ③ USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
- ④ MOUNTING HEIGHT OF 5 FEET FROM SIDEWALK SURFACE TO BOTTOM OF SIGN.
- ⑤ PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL IN THE SIDEWALK TERRACE.
- ⑥ IF TEMPORARY PEDESTRIAN BARRICADE DOES NOT REACH THE FACE OF THE CURB, USE AN ADDITIONAL PANEL AND EXTEND INTO THE TERRACE.
- ⑦ 4 FEET MINIMUM, 5 FEET DESIRABLE
- ⑧ IF MINIMUM LANE WIDTHS CAN'T BE ATTAINED, CURB RAMPS MAY NEED TO BE CONSTRUCTED AT SEPARATE TIMES.

LEGEND







-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL DRUM
-  WORK AREA
-  TEMPORARY CURB RAMP
-  TEMPORARY PEDESTRIAN SURFACE "A"
-  TEMPORARY PEDESTRIAN SURFACE "B"
-  TEMPORARY DETECTABLE WARNING FIELD
-  TEMPORARY PEDESTRIAN BARRICADE
-  OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC



**CURB RAMP PEDESTRIAN TRAFFIC CONTROL
SIDEWALK ON SINGLE SIDE**

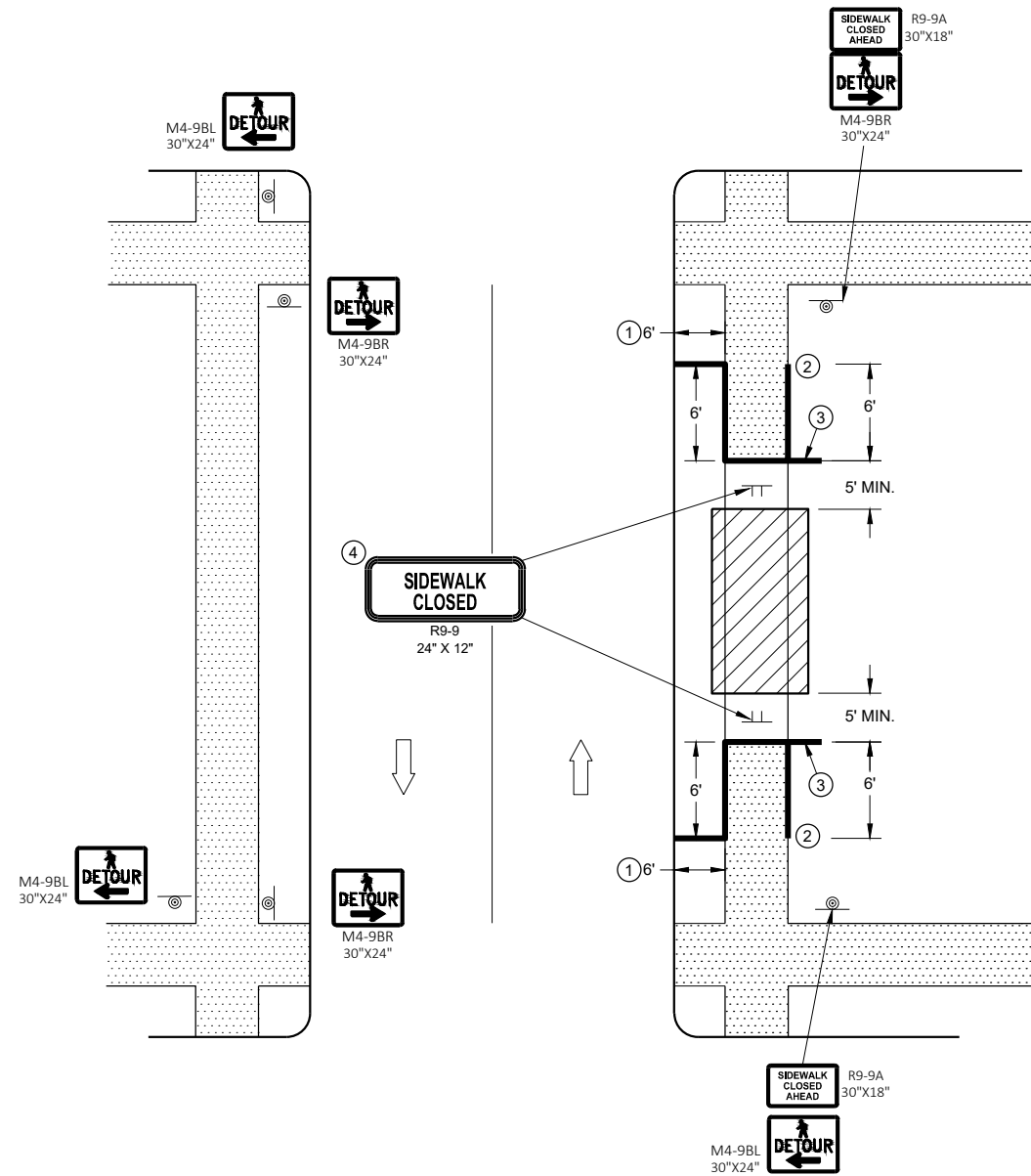
TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  SIGN ON TEMPORARY SUPPORT
-  UNDER PEDESTRIAN TRAFFIC
-  WORK AREA
-  TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC

GENERAL NOTES

- SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.
- WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.
- SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.
- PLACE TEMPORARY PEDESTRIAN BARRICADE TO FIT FIELD CONDITIONS, AVOIDING CONFLICT WITH DRIVEWAYS AND OTHER EXISTING FEATURES.
- ① IF TERRACE IS LESS THAN 6 FEET WIDE, OMIT TEMPORARY PEDESTRIAN BARRICADE FROM THE SIDEWALK TO THE CURB.
 - ② PLACE BARRICADE CLOSURE SO THAT THE TEMPORARY PEDESTRIAN BARRICADE END IS AT THE LAST OPEN SIDEWALK ACCESS TO RESIDENCES OR BUSINESSES BEFORE THE SIDEWALK CLOSURE.
 - ③ IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
 - ④ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.



SIDEWALK DETOUR, SIDEWALK ON BOTH SIDES

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

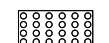
TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.

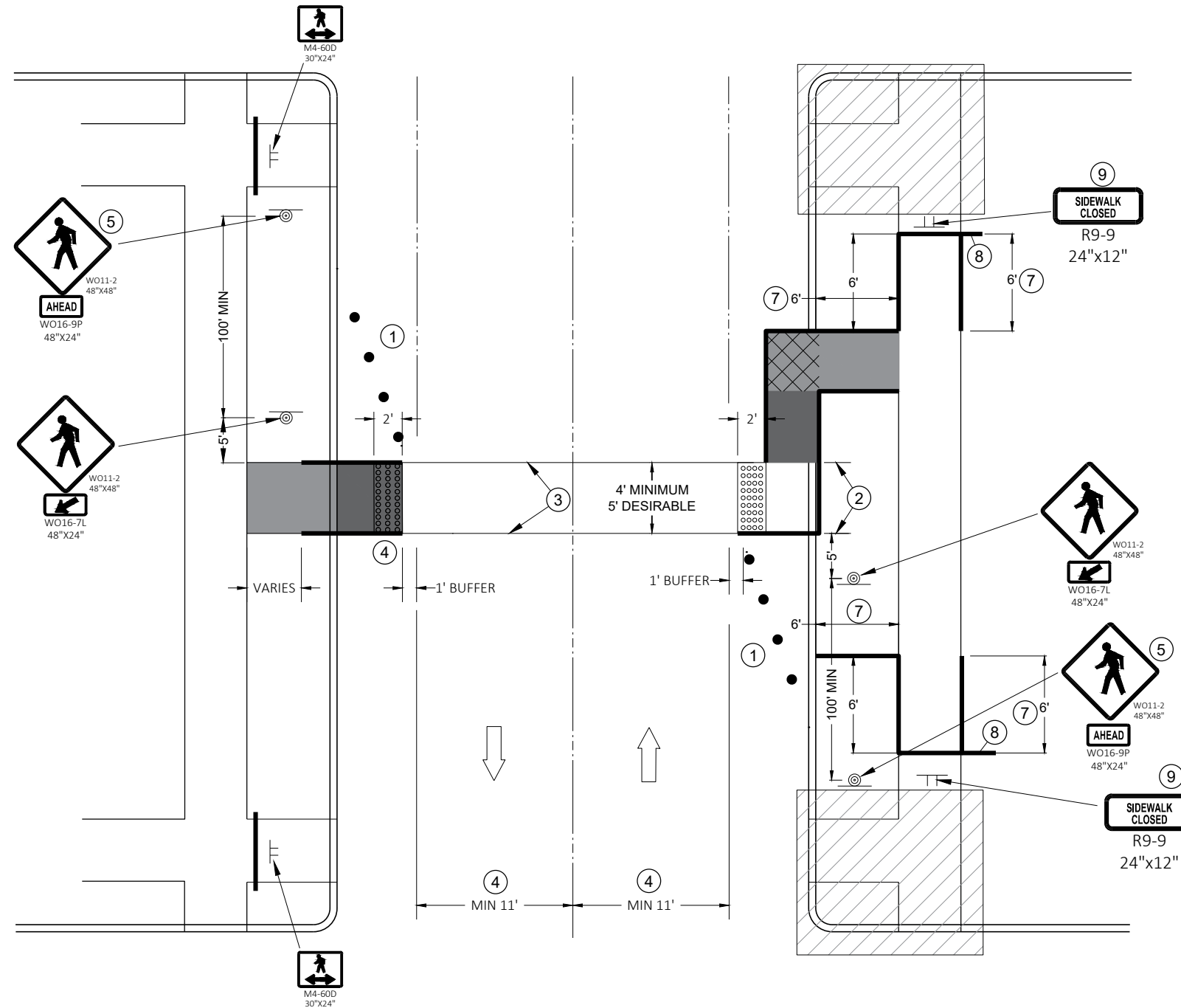
SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

SEE OTHER PEDESTRIAN ACCOMMODATION DETAILS FOR SIGNING AND DEVICES FOR DIFFERENT PEDESTRIAN FACILITIES CLOSURES.

- ① SHOULDER OR LANE CLOSURE ADVANCED WARNING AND PROPER BUFFER SPACE REQUIRED.
- ② 4 FEET MINIMUM, 5 FEET DESIRABLE.
- ③ WHITE 6" TEMPORARY PAVEMENT MARKING.
- ④ IF MINIMUM LANE WIDTHS CAN'T BE ATTAINED, PERPENDICULAR CURB RAMPS MAY NEED TO BE UTILIZED.
- ⑤ IF MINIMUM 100' SPACING FROM THE MID-BLOCK CROSSING CANNOT BE ATTAINED BEFORE THE INTERSECTION, REMOVE THIS SIGN ASSEMBLY.
- ⑥ IF TERRACE IS LESS THAN 6 FEET WIDE, OMIT TEMPORARY PEDESTRIAN BARRICADE FROM THE SIDEWALK TO THE CURB.
- ⑦ PLACE BARRICADE CLOSURE SO THAT THE TEMPORARY PEDESTRIAN BARRICADE END IS AT THE LAST OPEN SIDEWALK ACCESS TO RESIDENCES OR BUSINESSES BEFORE THE SIDEWALK CLOSURE.
- ⑧ IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF THE EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
- ⑨ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF THE SIGN.

LEGEND

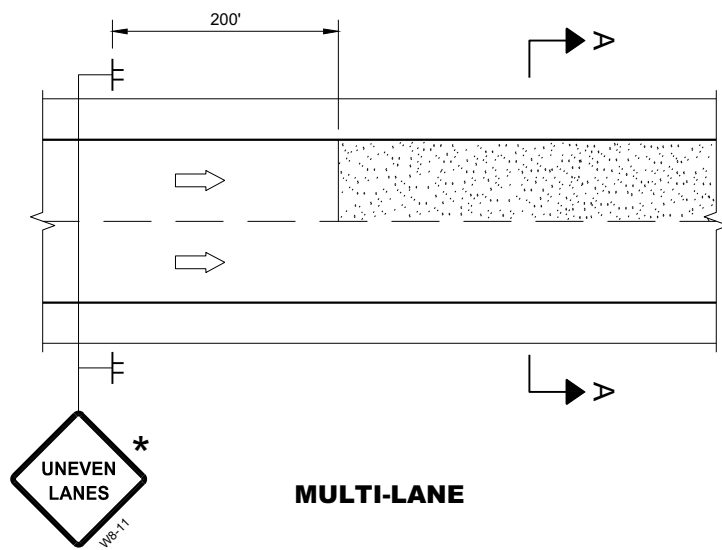
	TRAFFIC CONTROL DRUM
	SIGN ON TEMPORARY SUPPORT
	TEMPORARY CURB RAMP
	TEMPORARY DETECTABLE WARNING FIELD
	TEMPORARY PEDESTRIAN SURFACE "A"
	TEMPORARY PEDESTRIAN SURFACE "B"
	WORK AREA
	TEMPORARY PEDESTRIAN BARRICADE
	DIRECTION OF TRAFFIC



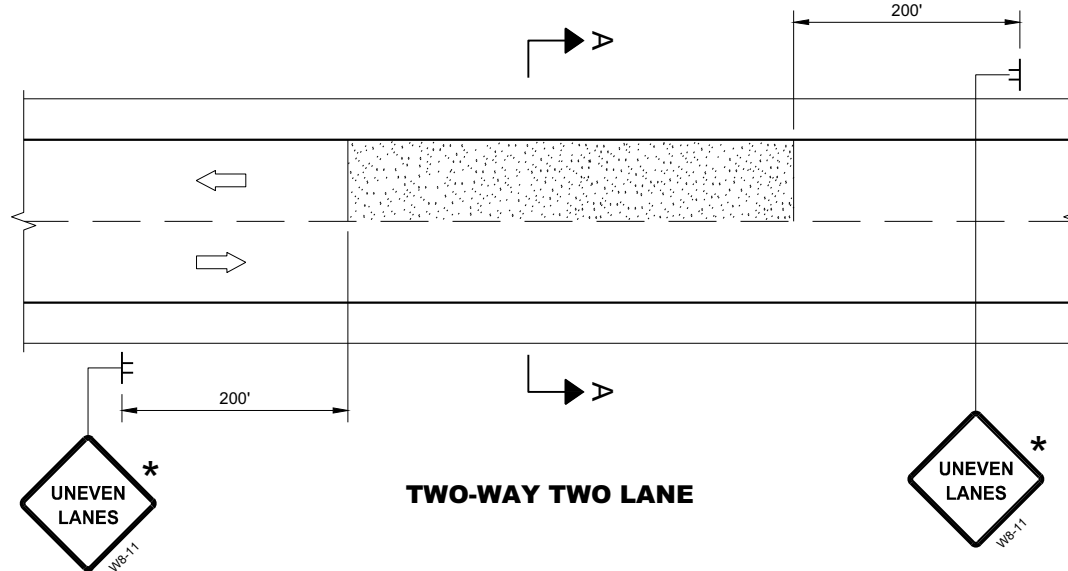
TEMPORARY PEDESTRIAN CROSSING

**TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION**

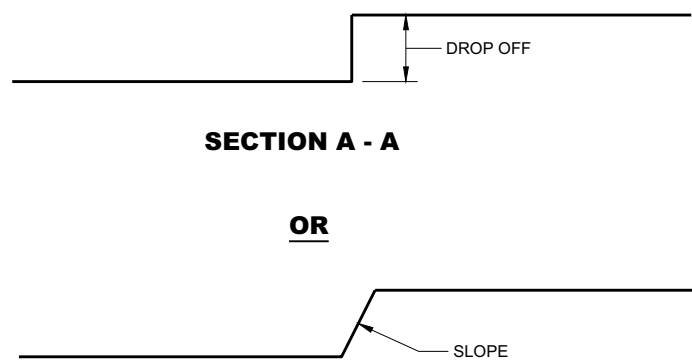
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



MULTI-LANE



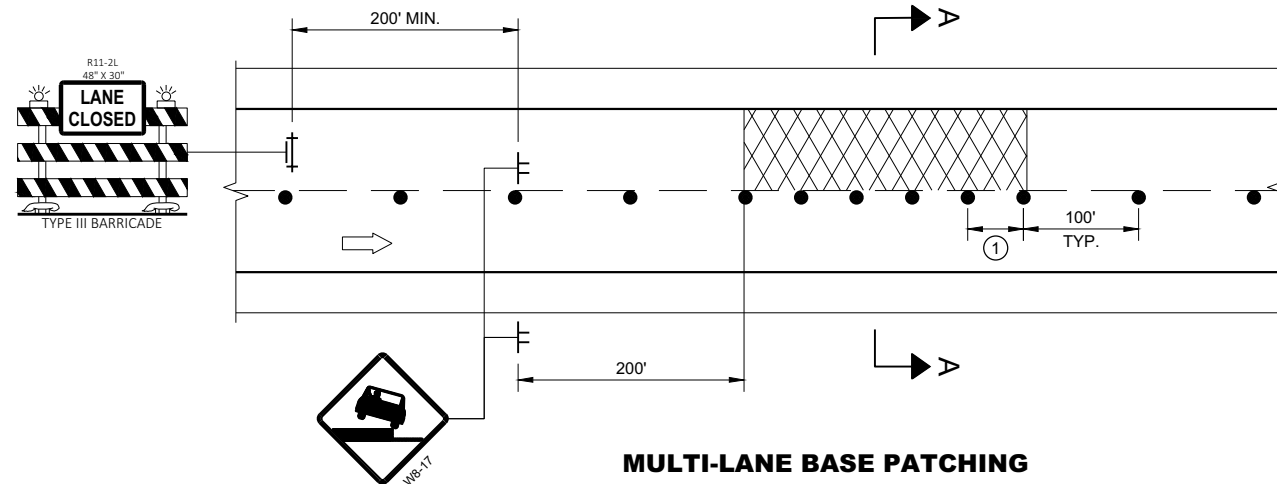
TWO-WAY TWO LANE



SECTION A - A

OR

SECTION A - A



MULTI-LANE BASE PATCHING

ADJACENT LANE DROP-OFFS

GENERAL NOTES

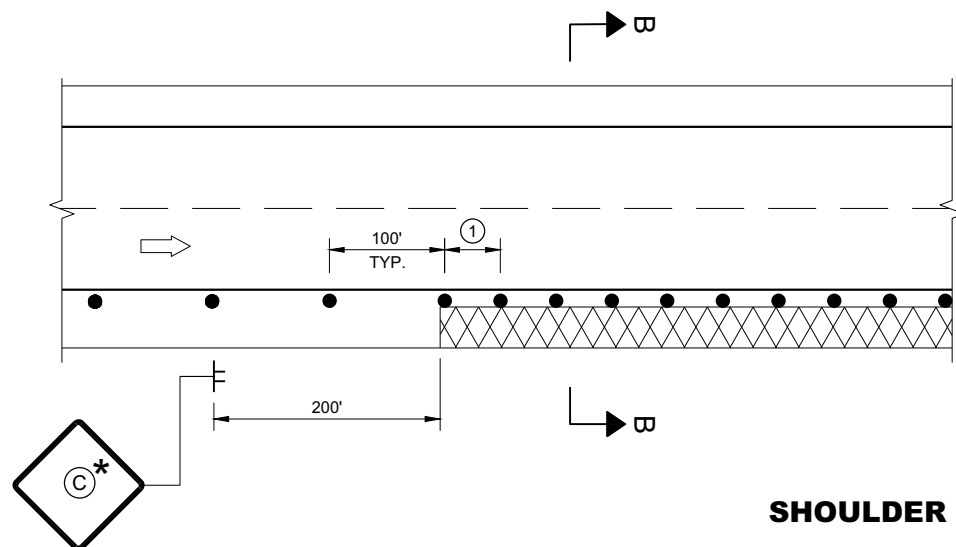
- FOR SPOT LOCATIONS USE ENGINEERING JUDGEMENT WHEN PLACING ADDITIONAL SIGNS.
- ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.
- "WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.
- WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.
- * IF THE DROP-OFF IS CONTINUOUS ALONG THE PROJECT, PLACE ADDITIONAL SIGNS EVERY 1 MILE AND AFTER EVERY ENTRANCE RAMP.
- ① USE CLOSER SPACING WHEN DELINEATING DROP-OFF.

LEGEND

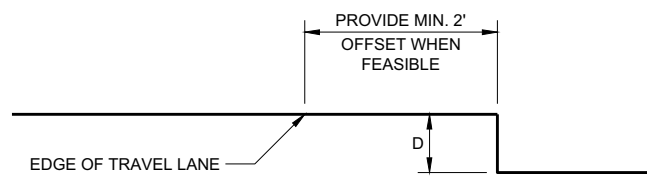
- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL DRUM
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- DIRECTION OF TRAFFIC
- WORK AREA WITH DROP-OFF
- MILLED SURFACE

6

6



SHOULDER DROP-OFFS



SECTION B - B

D	SIGN (C)
< 2" WITH A SLOPE STEEPER THAN 3:1	 LOW SHOULDER WO8-9
2" < 6" WITH A SLOPE STEEPER THAN 3:1	 SHOULDER DROP - OFF W8-9A PROVIDE A 3:1 OR FLATTER SLOPE OF MATERIAL ADJACENT TO THE PAVEMENT

SDD 15D39 - 02

SDD 15D39 - 02

**TRAFFIC CONTROL,
DROP-OFF SIGNING**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
March 2018 /S/ Andrew Heidtke
DATE DATE WORK ZONE ENGINEER

FHWA

GENERAL NOTES

DRAWING NOT TO SCALE. ALL SIGNS AND POSTS ON THIS SHEET SHALL BE PAID FOR WITH 'TRAFFIC CONTROL SIGNS' BID ITEM. ALL SIDE ROADS WHICH ARE UNDER CONSTRUCTION OF CURB AND GUTTER AND/OR GRADING SHALL BE ADEQUATELY SIGNED.

ALL SIGNS AND DEVICES SHALL BE IN CONFORMANCE WITH THE WISCONSIN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD). SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISDOT STANDARD SIGN PLATES.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THAT THE BACKGROUND IS ORANGE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.



THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

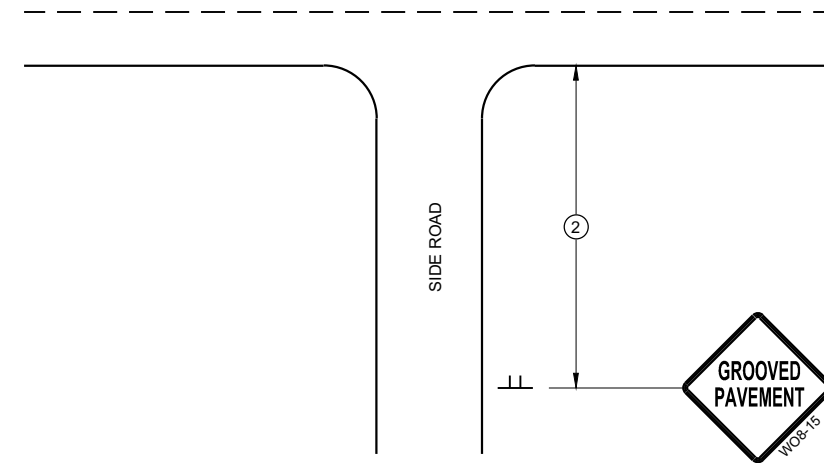
ALL SIGNS INAPPROPRIATE TO THE STATUS OF THE CONTROL ZONE, INCLUDING PRE-EXISTING SIGNS IN THE VICINITY, SHALL BE COVERED OR REMOVED AS DIRECTED BY THE ENGINEER.

SEE 15C34 FOR ADDITIONAL TRAFFIC CONTROL SIGNING WHEN CENTERLINE PAVEMENT MAKINGS ARE MISSING. 'DO NOT PASS' SIGNS MUST BE INSTALLED ON THE SAME DAY AS MILLING OPERATIONS.

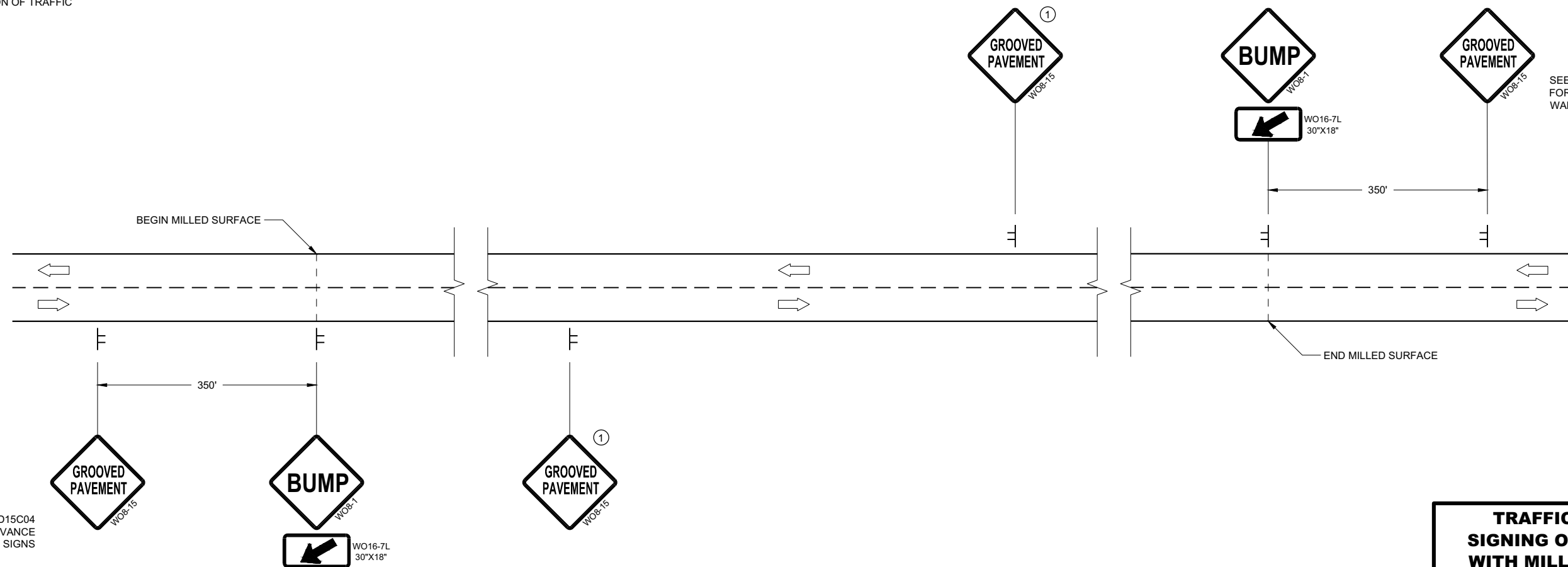
- ① PLACE SIGNS 350' IN ADVANCE OF MILLED SURFACES AND AT 1 MILE INTERVALS, OR AS DIRECTED BY THE ENGINEER.
- ② PLACE SIGN 200' MIN. FROM INTERSECTION AND 200' MIN. AFTER ADVANCE WARNING SIGN SHOWN IN SDD 15C04.

LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  DIRECTION OF TRAFFIC



TYPICAL SIDE ROAD APPROACH SIGN DETAIL



DETAIL FOR SIGNING ON MILLED SURFACES

TRAFFIC CONTROL, SIGNING ON ROADWAYS WITH MILLED SURFACES	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED February 2020 DATE	/s/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

GENERAL NOTES

DRAWING NOT TO SCALE. ALL SIGNS AND POSTS ON THIS SHEET SHALL BE PAID FOR WITH 'TRAFFIC CONTROL SIGNS' BID ITEM. ALL SIDE ROADS WHICH ARE UNDER CONSTRUCTION OF CURB AND GUTTER AND/OR GRADING SHALL BE ADEQUATELY SIGNED.

ALL SIGNS AND DEVICES SHALL BE IN CONFORMANCE WITH THE WISCONSIN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD). SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISDOT STANDARD SIGN PLATES.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THAT THE BACKGROUND IS ORANGE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

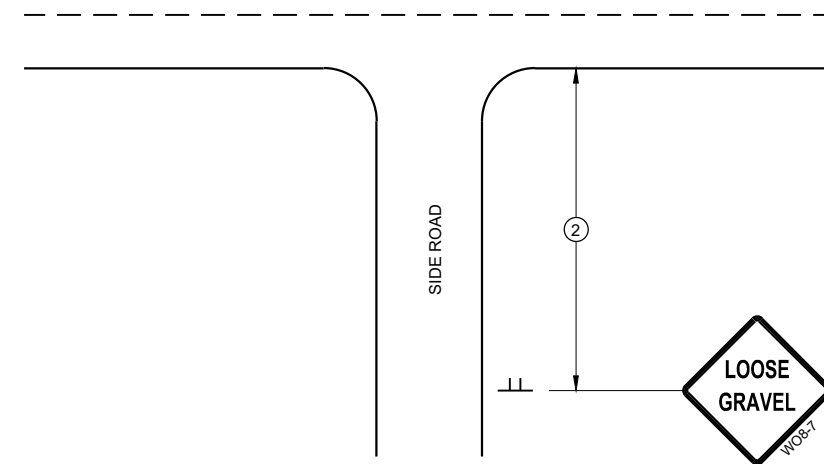
ALL SIGNS INAPPROPRIATE TO THE STATUS OF THE CONTROL ZONE, INCLUDING PRE-EXISTING SIGNS IN THE VICINITY, SHALL BE COVERED OR REMOVED AS DIRECTED BY THE ENGINEER.

SEE 15C34 FOR ADDITIONAL TRAFFIC CONTROL SIGNING WHEN CENTERLINE PAVEMENT MAKINGS ARE MISSING. 'DO NOT PASS' SIGNS MUST BE INSTALLED ON THE SAME DAY AS MILLING OPERATIONS.

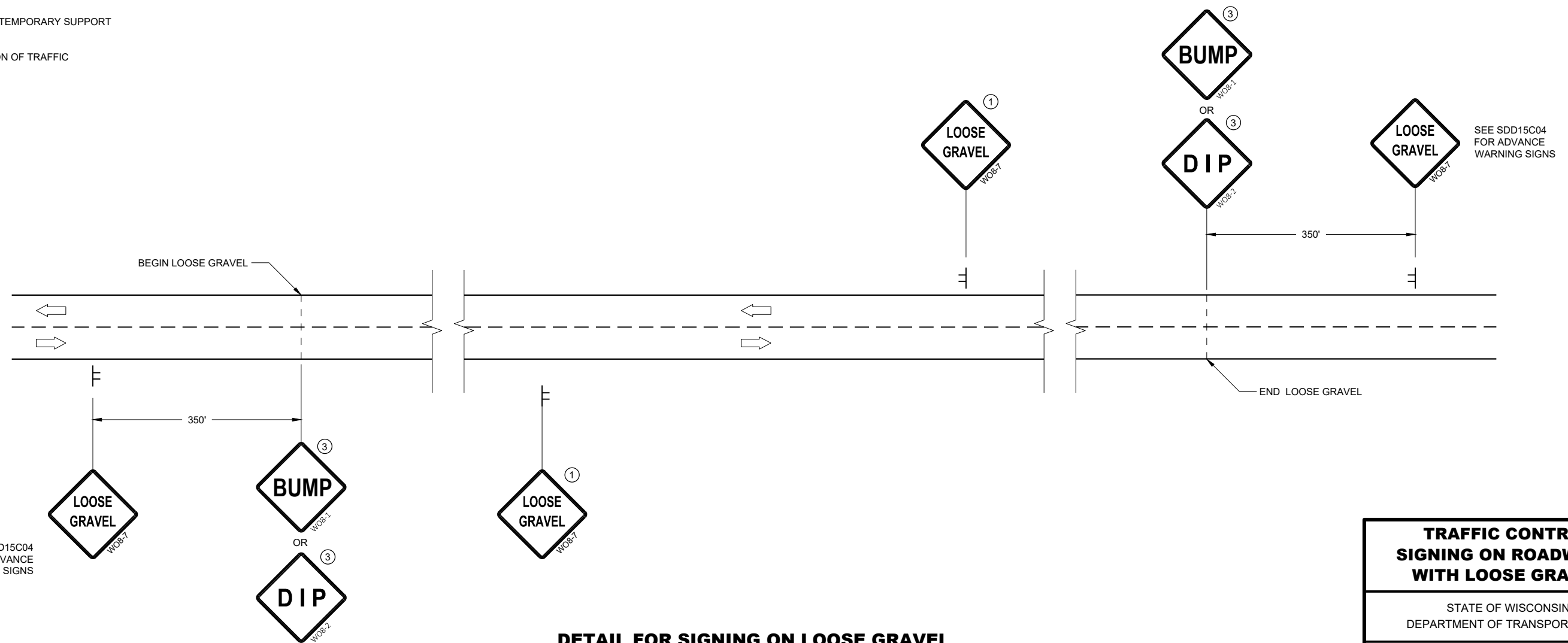
- ① PLACE SIGNS 350' IN ADVANCE OF CHIP SEALED OR LOOSE GRAVEL SURFACES AND AT 1 MILE INTERVALS, OR AS DIRECTED BY THE ENGINEER.
- ② PLACE SIGN 200' MIN. FROM INTERSECTION AND 200' MIN. AFTER ADVANCE WARNING SIGN SHOWN IN SDD 15C04.
- ③ ADD WO8-1 OR WO8-2 SIGN WHEN THE CONDITION IS PRESENT.

LEGEND

- ⊥ SIGN ON TEMPORARY SUPPORT
- ➡ DIRECTION OF TRAFFIC







TYPICAL SIDE ROAD APPROACH SIGN DETAIL



DETAIL FOR SIGNING ON LOOSE GRAVEL OR CHIP SEALED SURFACES

TRAFFIC CONTROL SIGNING ON ROADWAYS WITH LOOSE GRAVEL	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED February 2021 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
<small>FHWA</small>	

LEGEND

- V1 WORK VEHICLE
- V2 SHADOW VEHICLE
-  TRUCK MOUNTED ATTENUATOR (TMA)
-  FLASHING ARROW PANEL (CAUTION)
-  WORK AREA
-  DIRECTION OF TRAFFIC

POSTED SPEED PRIOR TO WORK STARTING (MPH)	DECISION SIGHT DISTANCE (D)
0 - 25	550'
30	550'
35	700'
40	700'
45	900'
50	900'
55	1200'

GENERAL NOTES

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED.

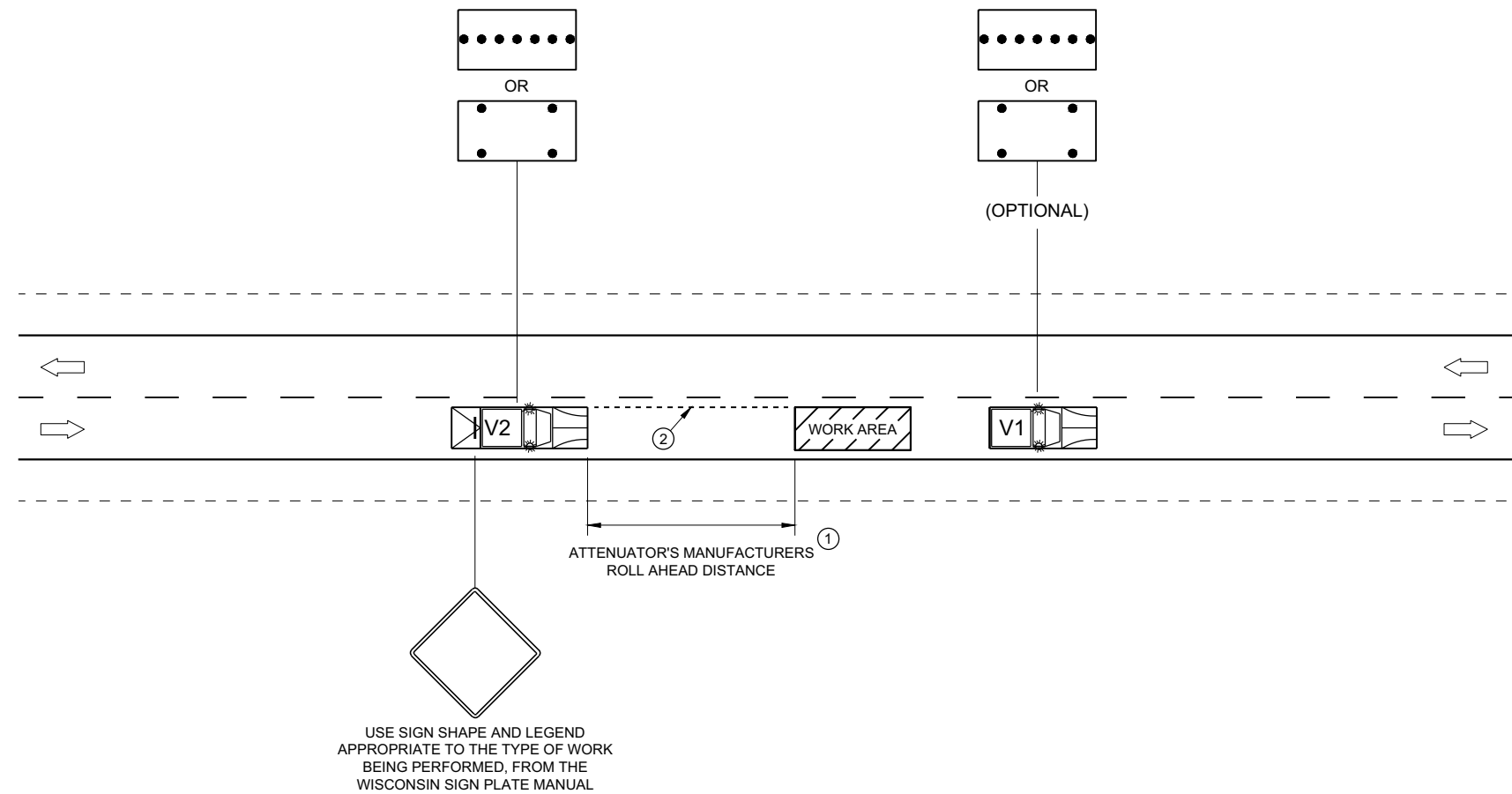
MOBILE IS WORK THAT MOVES CONTINUOUSLY OR MOVES AT LEAST THE DECISION SIGHT DISTANCE EVERY 15 MINUTES.

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

ALL ARROW PANELS SHALL BE REAR FACING, TYPE "B" OR "C", AND DISPLAYING THE FLASHING CAUTION MODE. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

USE AN ATTENUATOR ON THE REARMOST VEHICLE THAT BLOCKS ALL OR PART OF THE TRAFFIC LANE.

- ① DISTANCE BETWEEN VEHICLES MAY INCREASE FROM THE ATTENUATOR'S ROLL AHEAD BASED ON TERRAIN, SIGHT DISTANCE, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.
- ② ALIGN LEFT SIDE OF SHADOW VEHICLE WITH EDGE OF WORK AREA.



6

6

SDD 15D51 - 01

SDD 15D51 - 01

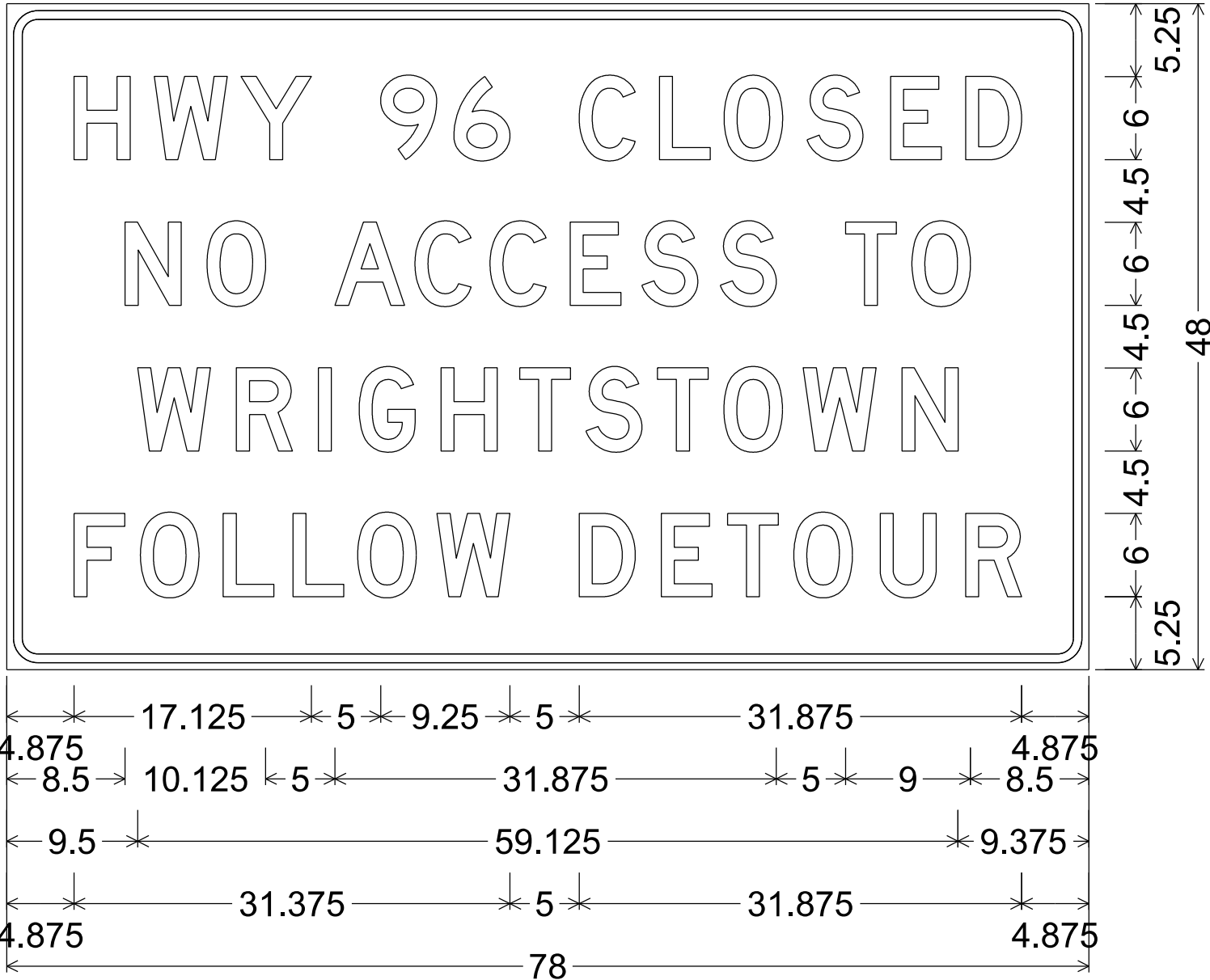
**TRAFFIC CONTROL,
MOBILE OPERATIONS ON
AN UNDIVIDED ROADWAY**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2021 DATE /S/ Andrew Heidtke
STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER
FHWA

NOTES

- 1. Fixed Message Sign Type II - Type F Reflective
- 2. Color:
 - Background - Orange
 - Message - Black
- 3. Message Series - D



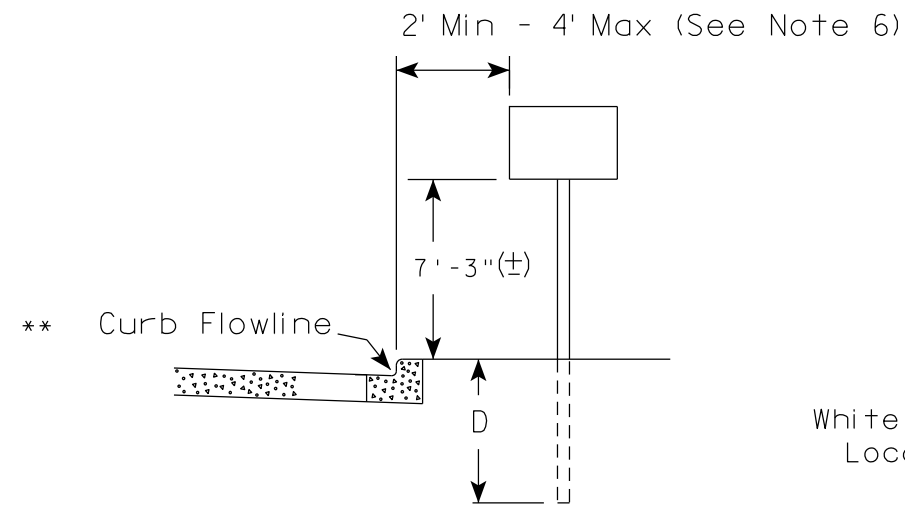
2.250" Radius, 0.625" Border, 0.500" Indent

7

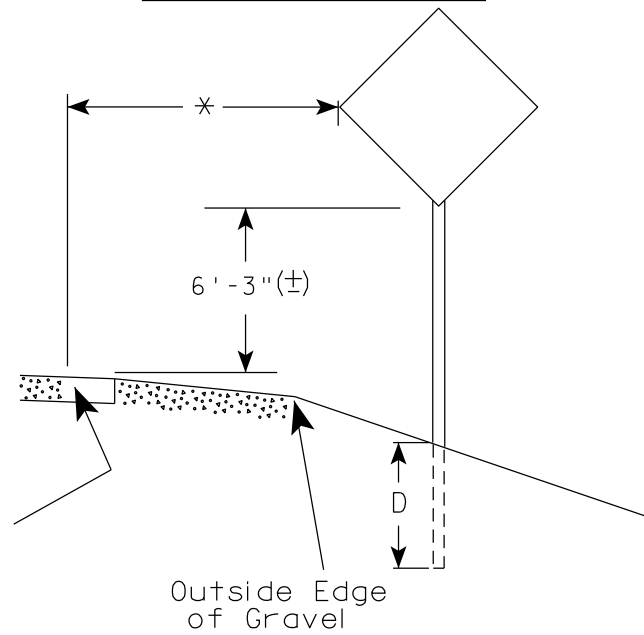
7

URBAN AREA

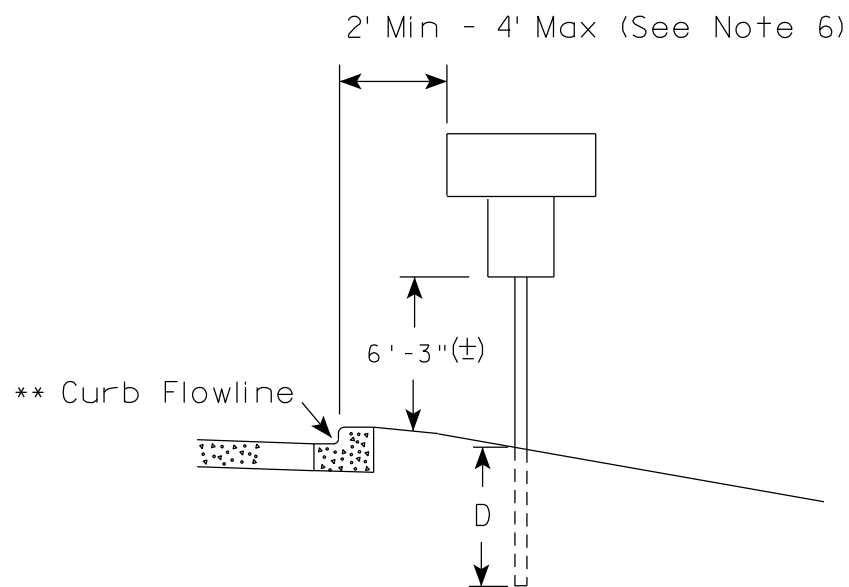
RURAL AREA (See Note 2)



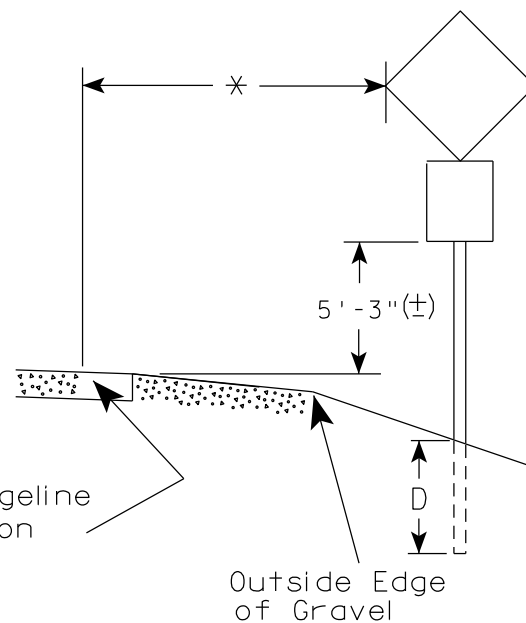
White Edgeline Location



Outside Edge of Gravel



White Edgeline Location



Outside Edge of Gravel

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

GENERAL NOTES

1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on or behind barrier wall, see A4-10 sign plate.
The Double Arrow sign (W12-1D) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).
3. For expressways and freeways, mounting height is 7'- 3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
4. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±).
5. Offset distance shall be consistent with existing signs or consistent throughout length of project.
6. The (±) tolerance for mounting height is 3 inches.
7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the Engineer.

7

7

* * The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

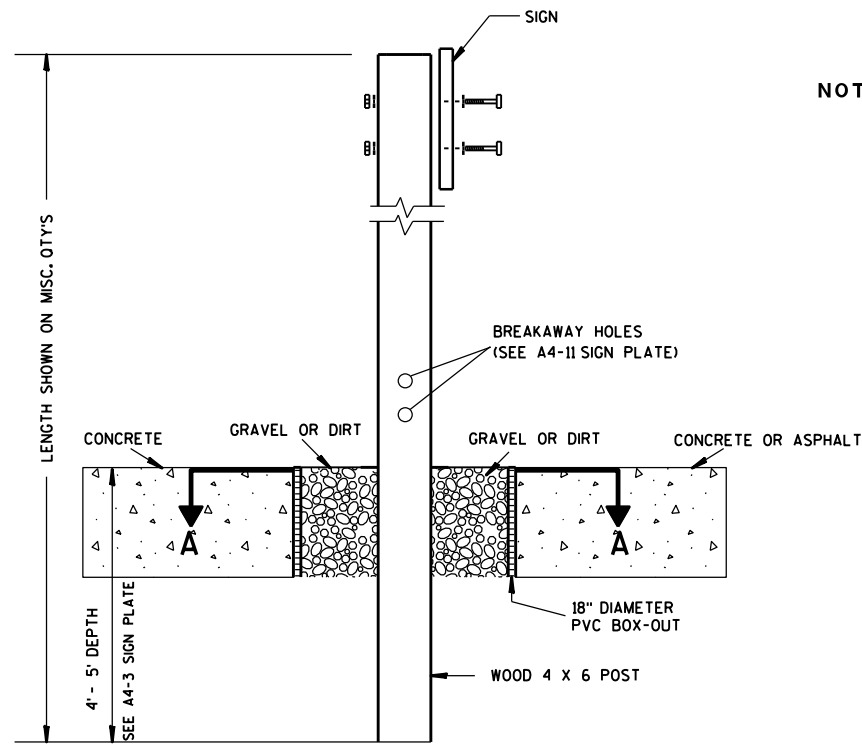
* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

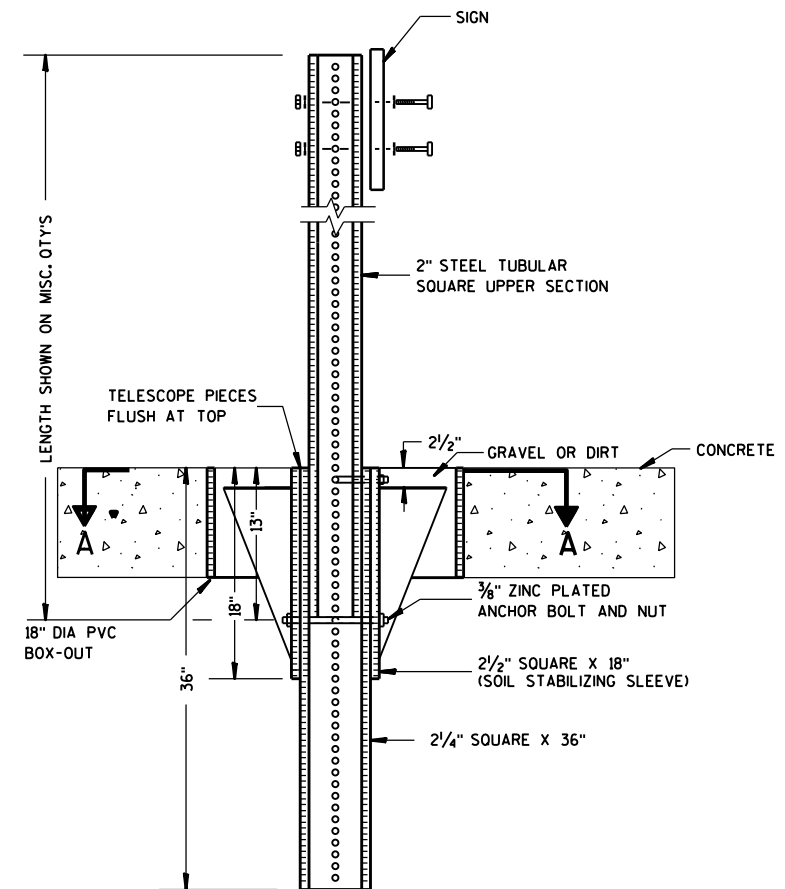
DATE 5/13/2020 PLATE NO. A4-3.22



ELEVATION VIEW

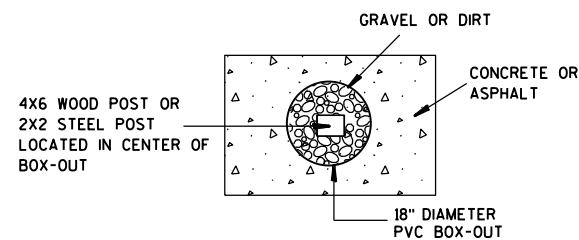
DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

- NOTES:**
1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
 2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
 3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



PLAN VIEW

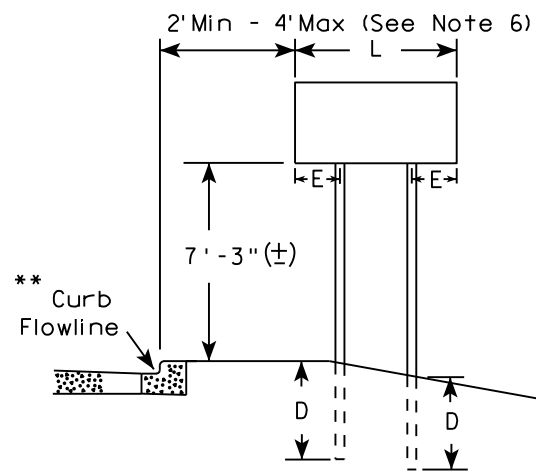
FOR NEW CONCRETE/ ASPHALT INSTALLATIONS

SIGN POST BOX-OUTS A4-3B	
<small>WISCONSIN DEPT OF TRANSPORTATION</small>	
APPROVED <i>Matthew R. Rauch</i> <small>for State Traffic Engineer</small>	
<small>DATE 1/27/14</small>	<small>PLATE NO. A4-3B.1</small>

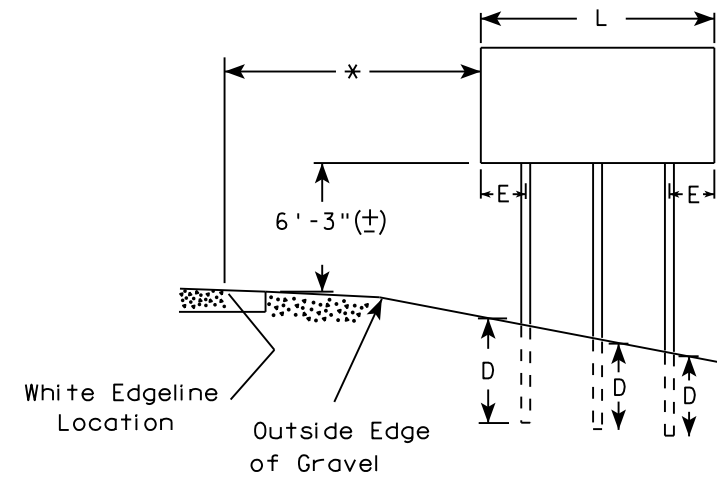
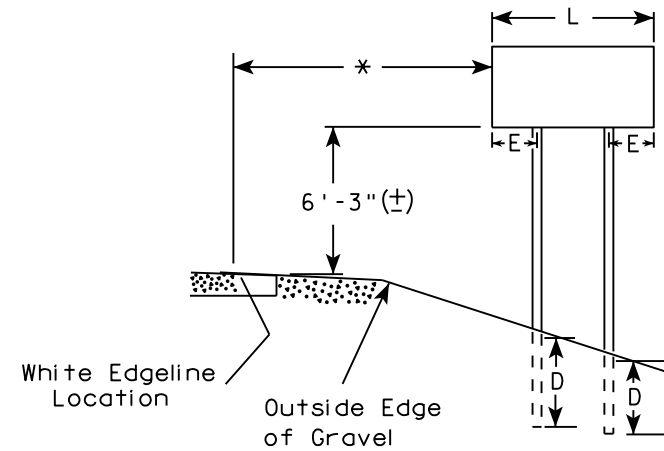
GENERAL NOTES

1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
2. See tables below for required number of posts.
3. For expressways and freeways, mounting height is 7'-3" (±) or 6'-3" (±) depending upon existence of sub-sign.
4. The (±) tolerance for mounting height is 3 inches.
5. J-Assemblies are considered to be one sign for mounting height.
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the engineer.
8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).

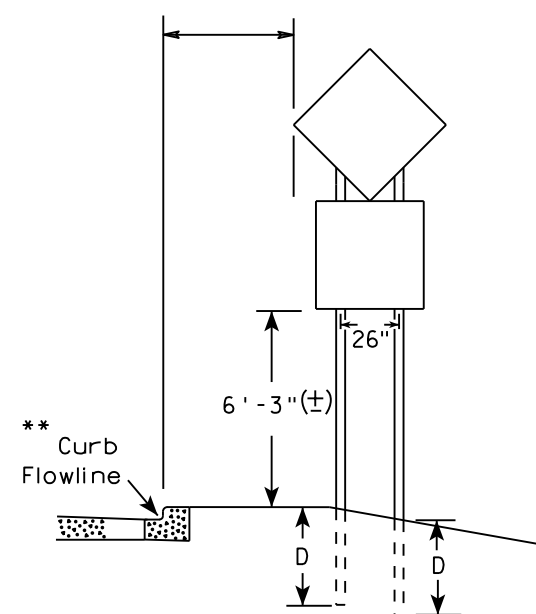
URBAN AREA



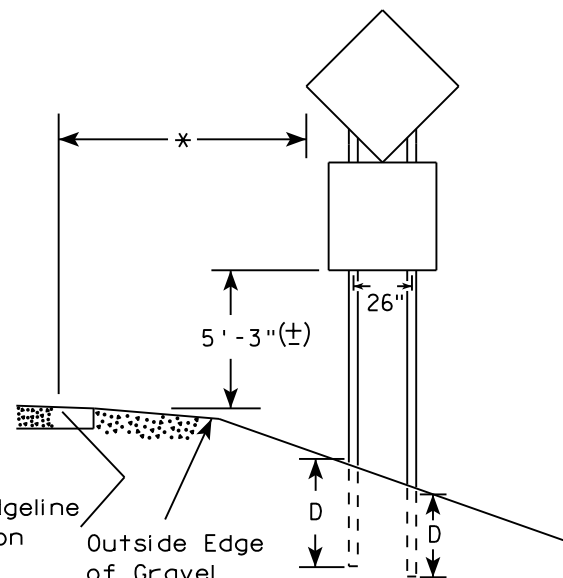
RURAL AREA (See Note 3)



2' Min - 4' Max (See Note 6)



48" DIAMOND WARNING SIGN



48" DIAMOND WARNING SIGN

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

*** See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)	
L	E
Greater than 48" Less than 60"	12"
60" to 108"	L/5

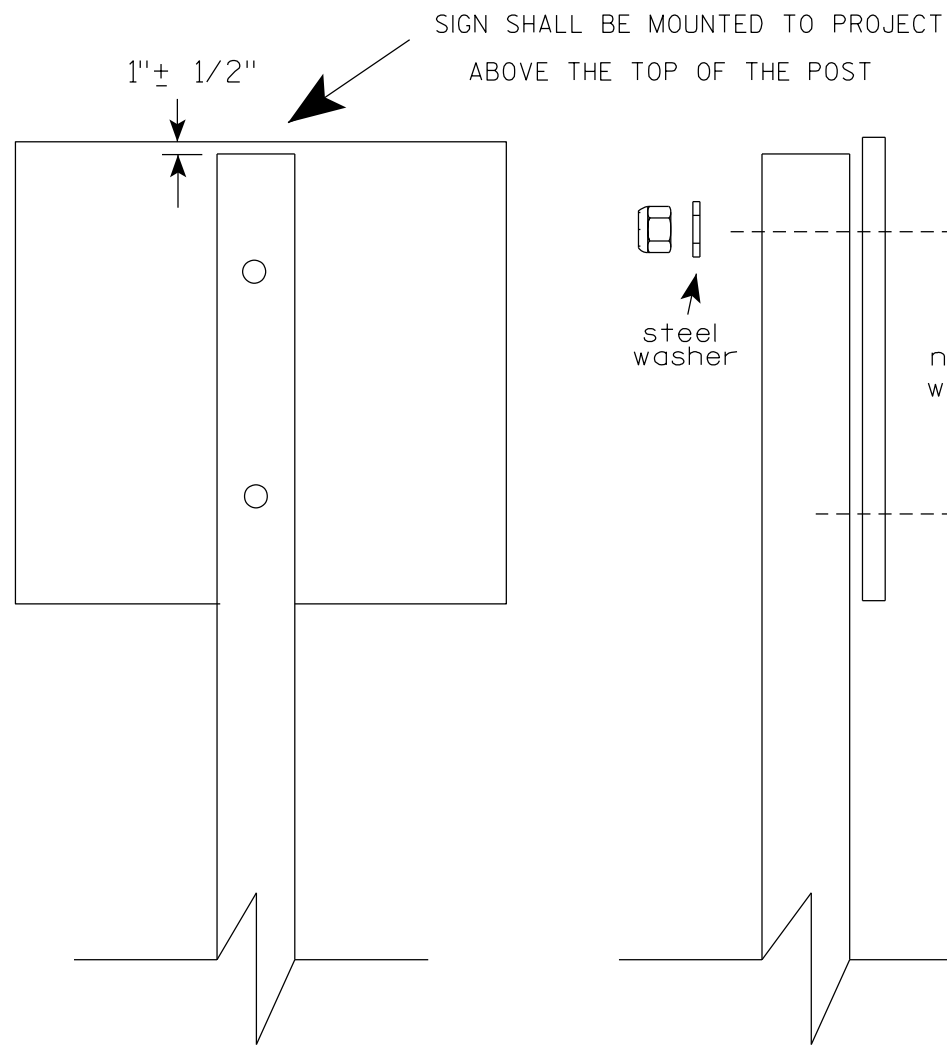
SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)	
L	E
Greater than 108" to 144"	12"

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS

WISCONSIN DEPT OF TRANSPORTATION
 APPROVED *Matthew R. Rauch*
 For State Traffic Engineer
 DATE 8/21/17 PLATE NO. A4-4.15



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

- a. Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
- b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)

MACHINE BOLTS - 5/16" X 1-3/4" Length w/ lock nuts

WOOD POSTS (4" x 6")

LAG SCREWS - 3/8" X 3" (NO STRINGERS ON BACK OF SIGN)
3/8" X 4" (STRINGERS ON BACK OF SIGN)

SQUARE STEEL POSTS (2" x 2")

MACHINE BOLTS - 3/8" X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)
3/8" X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)

RIVETS - 9/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH

WASHERS (ALL POSTS) -

1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL

1-1/4" O.D. X 3/8" I.D. X .080 NYLON

* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS
TO POSTS

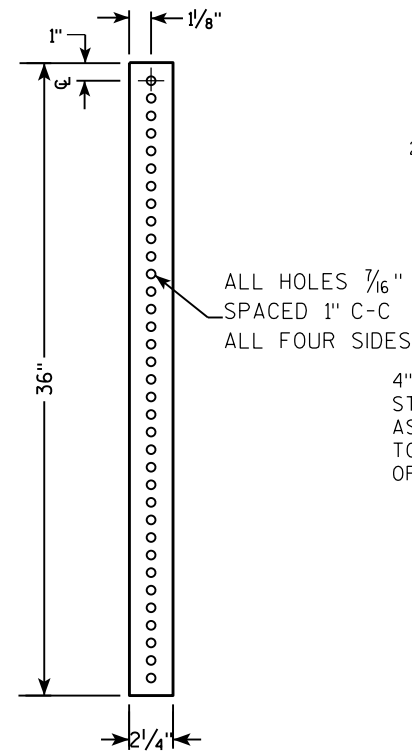
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Rauch*
For State Traffic Engineer

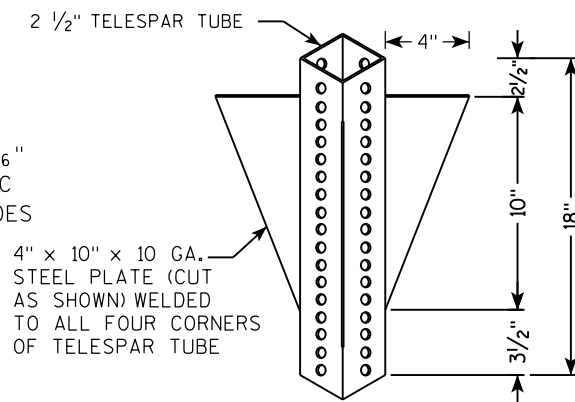
DATE 4/1/2020 PLATE NO. A4-8.9

**TELESCOPIC TUBING ANCHORS
TWO PIECE SYSTEM**

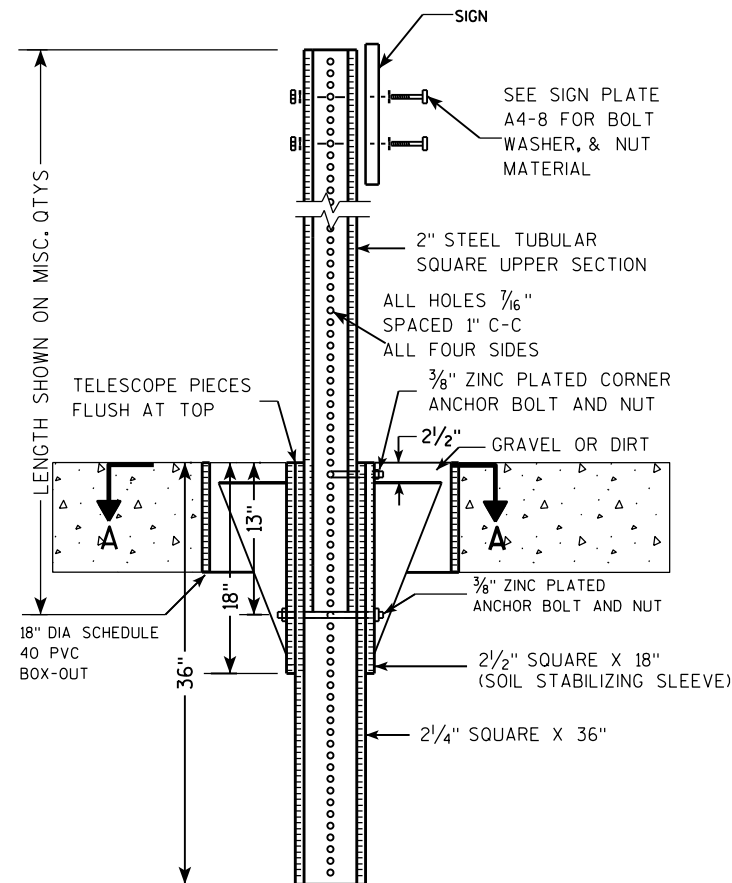
2 1/4" SQUARE
12 GAUGE
PERFORATED
GALVANIZED FINISH



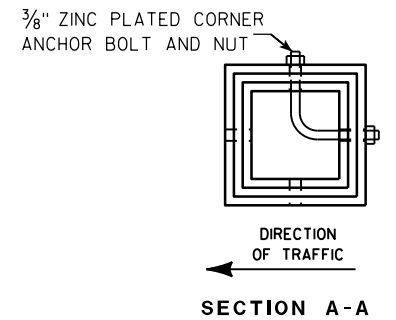
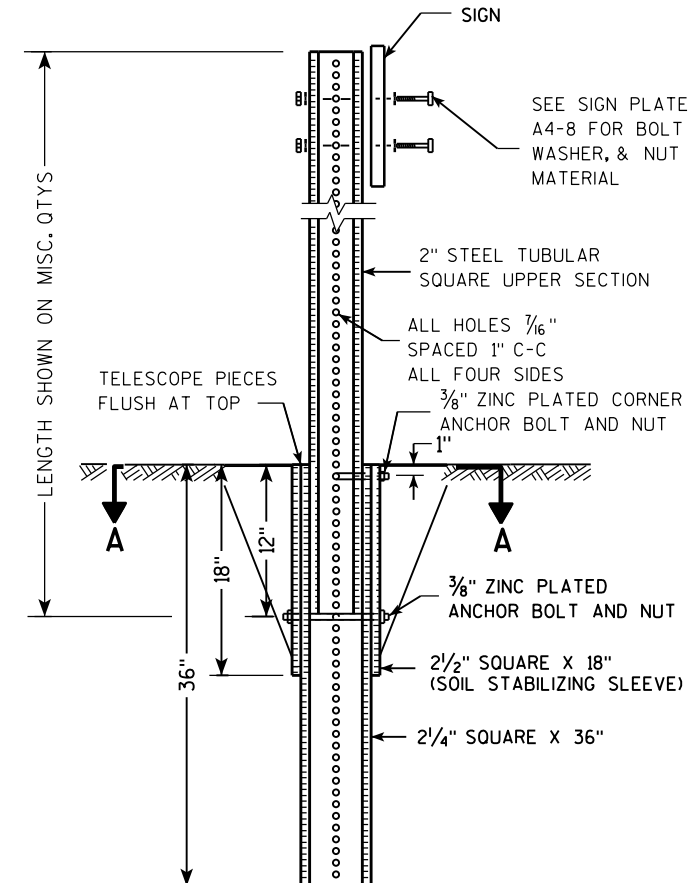
2 1/2" SQUARE
12 GAUGE
OMNI-DIRECTIONAL
PERFORATED
SOIL STABILIZING SLEEVE
GALVANIZED FINISH



**DETAIL OF TUBULAR STEEL SIGN POST
(IN POURED CONCRETE OR ASPHALT)**



**DETAIL OF TUBULAR STEEL SIGN POST
(IN LOCATIONS OTHER THAN POURED CONCRETE OR ASPHALT)**



Area of Sign Installation (Sq. Ft.)	Number of Required Posts
9 or less	1
Greater than 9 less than or equal to 18	2
Greater than 18 less than or equal to 27	3

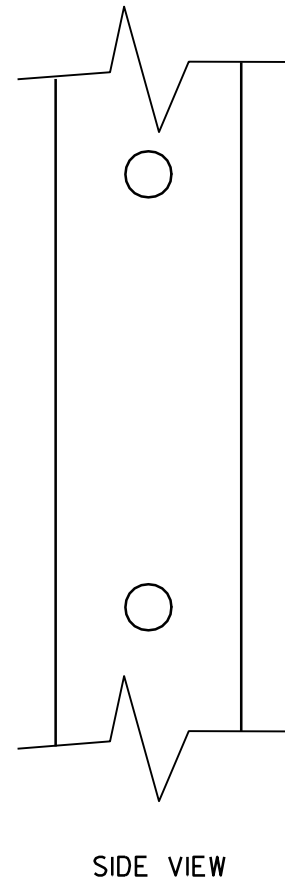
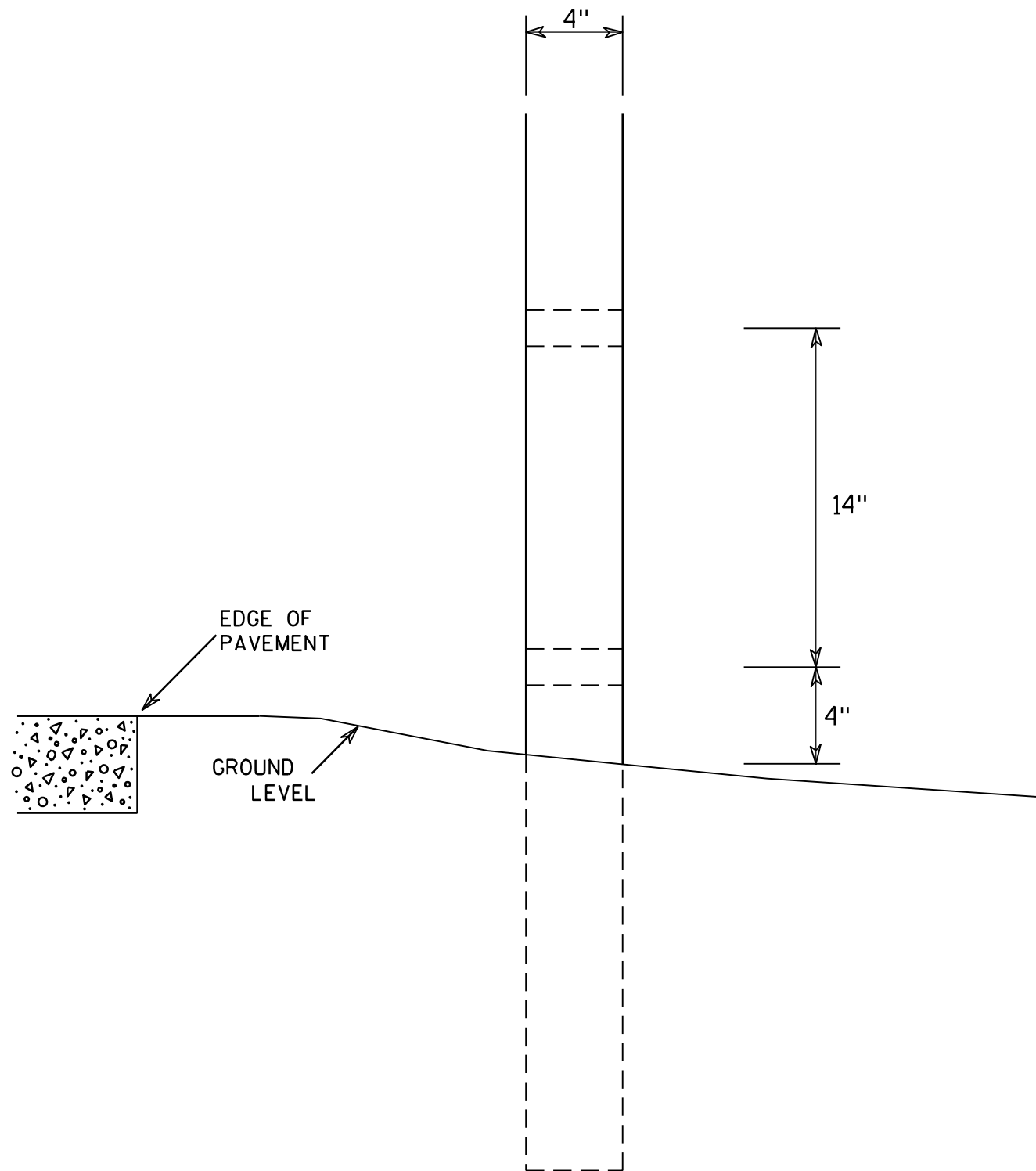
Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

**TUBULAR STEEL
SIGN POST
A4-9**

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 2/05/15 PLATE NO. A4-9.9




GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

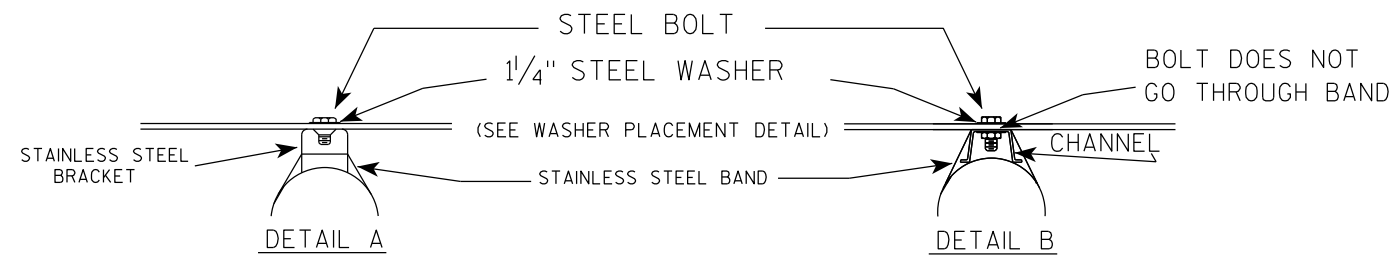
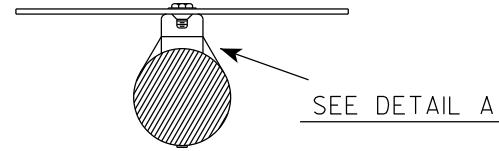
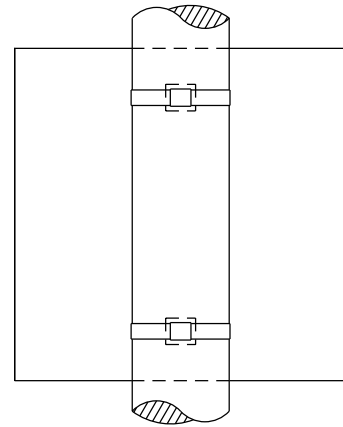
7

7

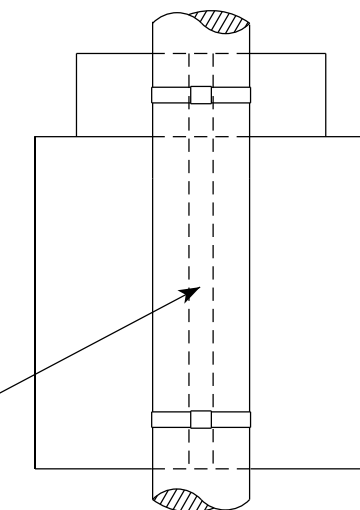
4 X 6 WOOD POST MODIFICATIONS	
<i>WISCONSIN DEPT OF TRANSPORTATION</i>	
APPROVED	 <i>for</i> State Traffic Engineer
DATE <u>3/27/97</u>	PLATE NO. <u>A4-11.2</u>

BANDING

SINGLE SIGN



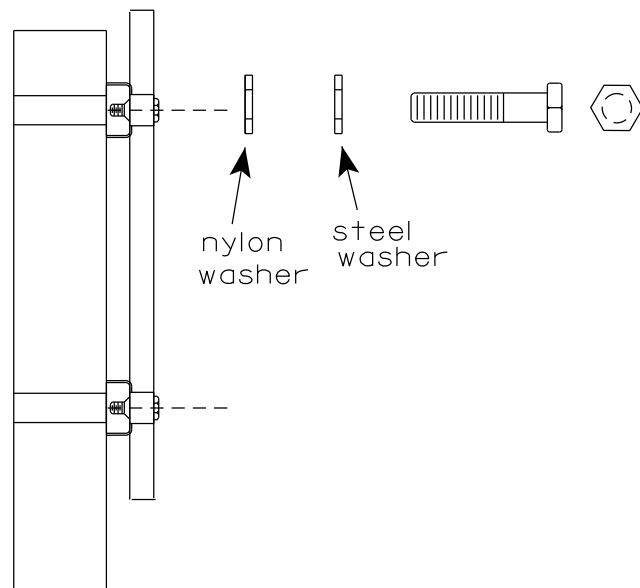
"J" ASSEMBLY



CHANNEL
SEE TYPICAL PANEL
INSTALLATION SHEET



WASHER PLACEMENT



WASHERS (ALL POSTS) -
1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
1-1/4" O.D. X 3/8" I.D. X .080 NYLON
FOR ALL TYPE H SIGNS

GENERAL NOTES

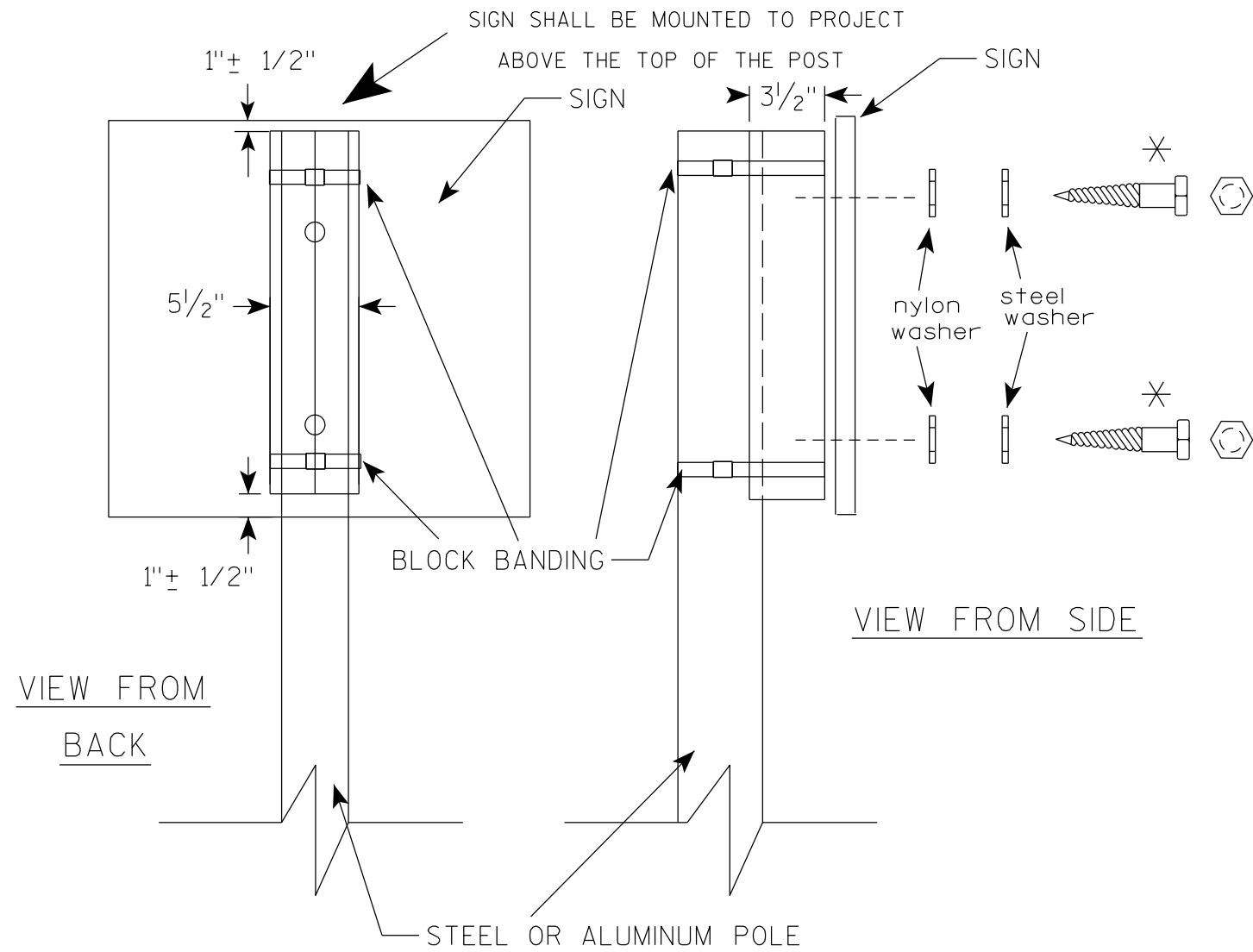
1. Any sign over 3 feet in width shall use the V-Block banding method. See A5-10 standard plate.
2. Signs 3 feet or greater in height shall have three bracket bands installed. Signs less than 3 feet in height shall have two bracket bands installed.
3. Banding and assembly bracket shall be stainless steel. All bands shall be 3/4" in width and 0.025" thickness.
4. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
 - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
 - b. Electro-galvanized in accordance with ASTM designation: B 633, Type III, SC 3

STANDARD SIGN
SIGN BANDING DETAILS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

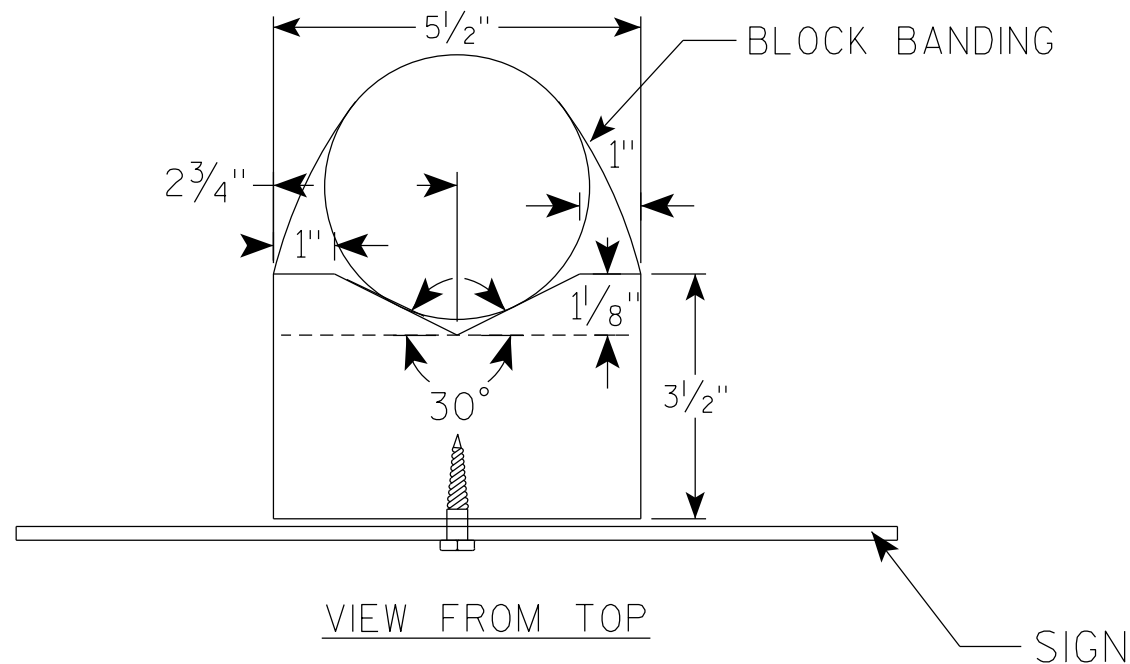
DATE 6/10/19 PLATE NO. A5-9.4



GENERAL NOTES

1. WOOD 4"X6" POST MATERIAL SHALL CONFORM TO 507.2.2 OF THE WISDOT STANDARD SPECIFICATIONS
2. BLOCK BANDING AND CLIPS SHALL BE STAINLESS STEEL, 3/4" WIDTH AND 0.025" THICKNESS
3. SIGNS 3' OR GREATER IN HEIGHT SHALL UTILIZE 3 BLOCK BANDS. SIGNS UNDER 3' IN HEIGHT SHALL UTILIZE 2 BLOCK BANDS
4. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA, BUT NORMALLY THERE ARE TWO. FOR SIGNS GREATER THAN 9 S.F. 3 FASTENERS SHALL BE USED.
5. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
 - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
 - b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3
6. ALL BOLTS SHALL HAVE HEXAGONAL HEADS.
7. STEEL WASHERS SHALL BE 1/4" O.D. X 3/8" I.D. X 1/16"
8. NYLON WASHERS SHALL BE 1/4" O.D. X 3/8" I.D. X .080 FOR TYPE H OR TYPE F FACE SIGN

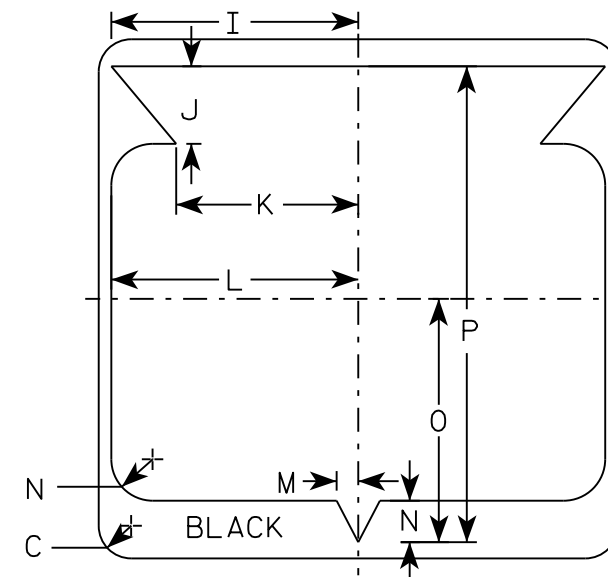
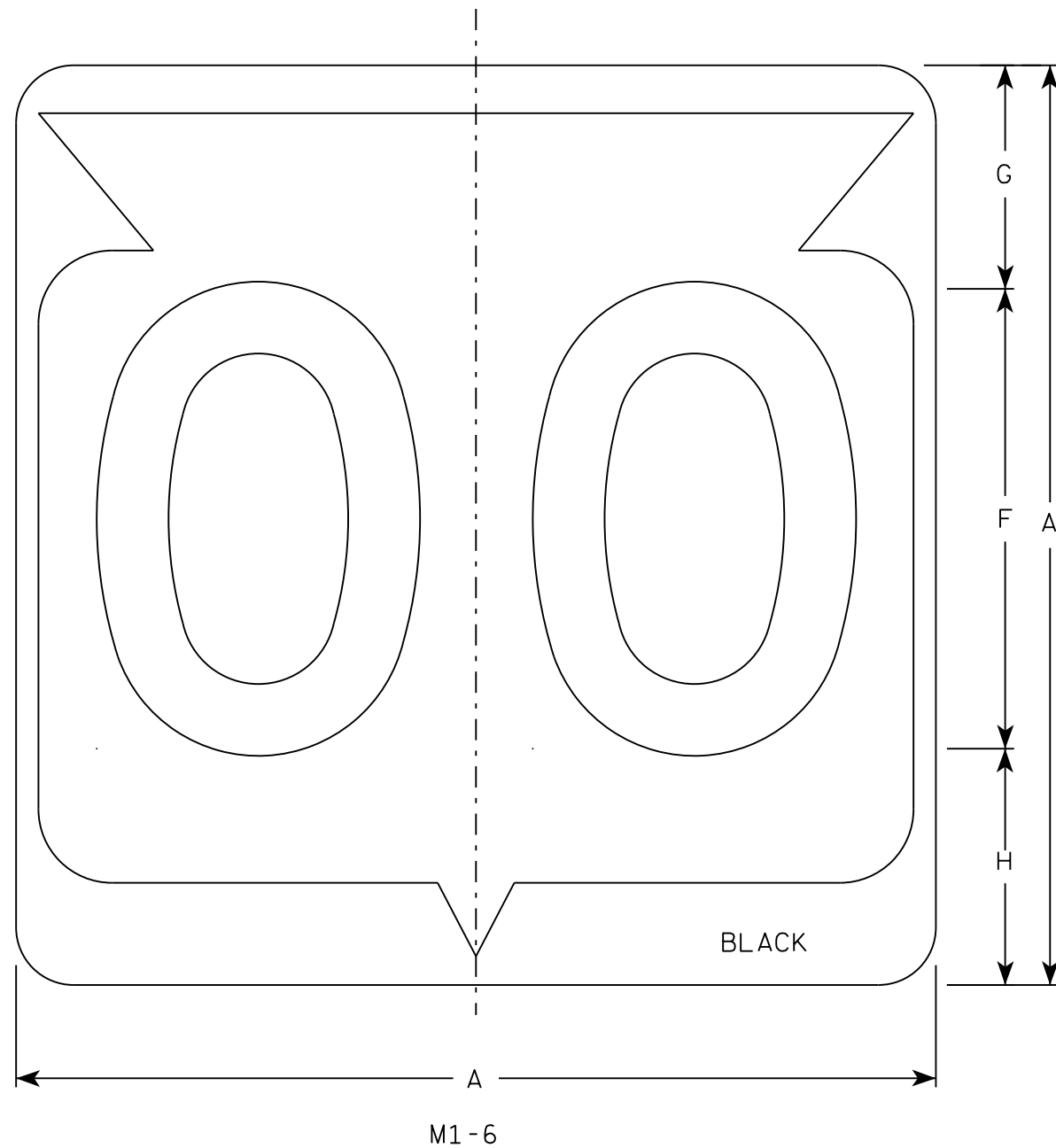
✱ LAG BOLTS SHALL BE 3/8" X 2 1/2"



BLOCK BANDING DETAIL (V-BLOCK OPTION)	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R Rauch</i> for State Traffic Engineer
DATE 4/19/2022	PLATE NO. A5-10.3

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - D except 3 number signs Series C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24		1 1/2			12	5 1/2	6 1/2	10 1/4	2 1/2	8 7/8	11 1/2	1	1 7/8	11 1/4	21 7/8											4.0
3	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0
4	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0
5	36		2 1/4			18	8 3/4	9 1/4	15 3/8	5 3/8	12 5/8	17 1/8	1 1/2	2 7/8	16 7/8	33											9.0

STATE ROUTE MARKER
M1-6 FOR ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 3/16/18 PLATE NO. M1-6.10

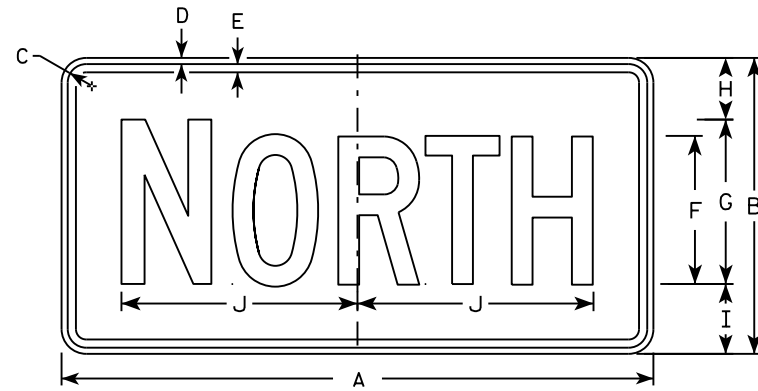
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ **E**

7

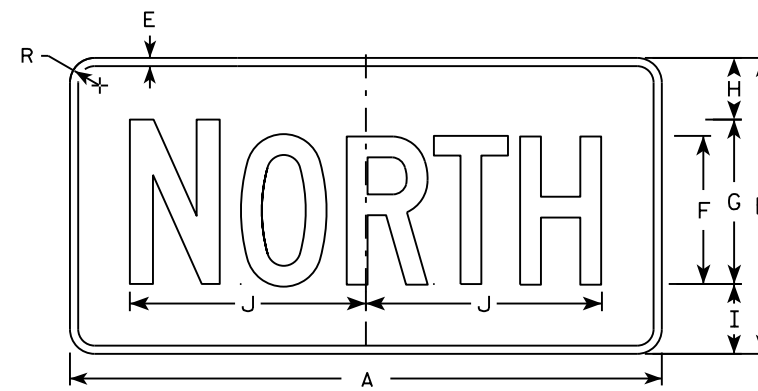
7

NOTES

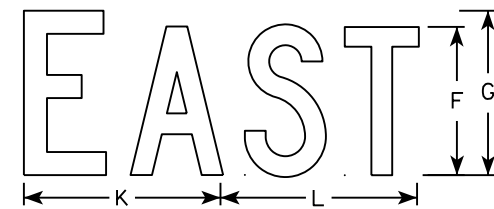
- All Signs Type II - Type H
- Color:
 - Background - See note 5
 - Message - See note 5
- Message Series - C
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- M3-1 thru M3-4 Background - White
 Message - Black
 MB3-1 thru MB3-4 Background - Blue
 Message - White
 MK3-1 thru MK3-4 Background - Green
 Message - White
 MM3-1 thru MM3-4 Background - White
 Message - Green
 MN3-1 thru MN3-4 Background - Brown
 Message - White
 MP3-1 thru MP3-4 Background - White
 Message - Blue
- Note the first letter of each direction is larger than the remainder of the message.



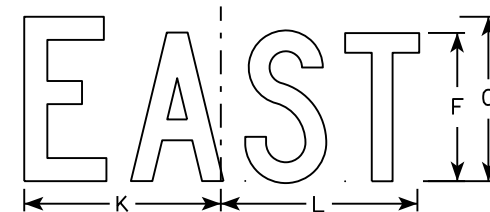
M3-1
MM3-1
MP3-1



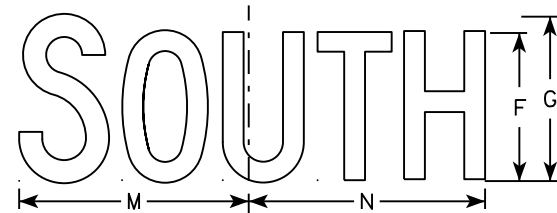
MB3-1
MK3-1
MN3-1



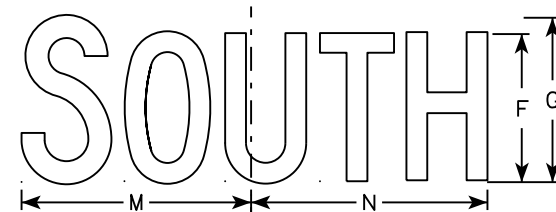
M3-2
MM3-2
MP3-2



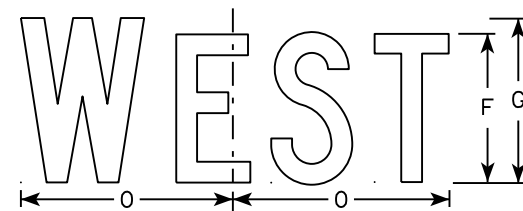
MB3-2
MK3-2
MN3-2



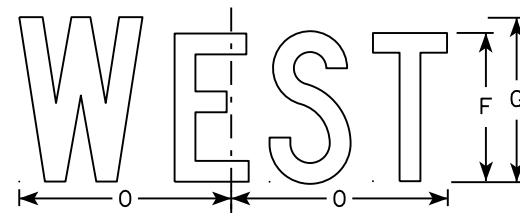
M3-3
MM3-3
MP3-3



MB3-3
MK3-3
MN3-3



M3-4
MM3-4
MP3-4



MB3-4
MK3-4
MN3-4

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	12	1 1/8	3/8	3/8	6	7	2 1/4	2 3/4	10 1/4	7 7/8	8 3/8	10 1/4	9 3/4	8 3/4			1 1/2									2.00
3	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
4	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
5	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5

STANDARD SIGNS
M3-1 thru M3-4
SERIES

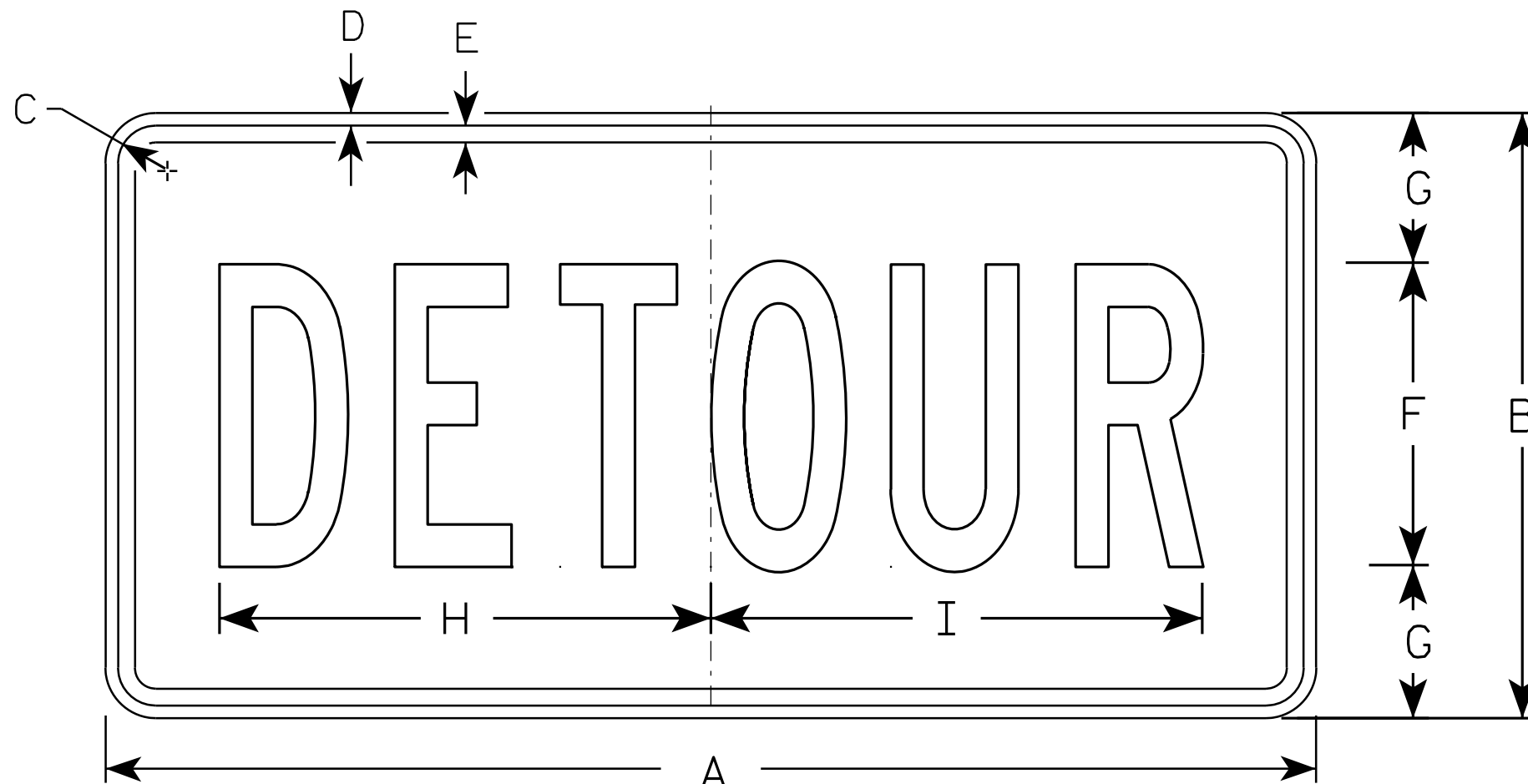
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 10/15/15 PLATE NO. M3-1.14

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - B
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



M4-8

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	12	1 1/8	3/8	3/8	6	3	10	10 1/4																		2.0
3	36	18	1 1/8	3/8	1/2	9	4 1/2	14 5/8	14 1/2																		4.5
4																											
5																											

STANDARD SIGN
M4-8

WISCONSIN DEPT OF TRANSPORTATION

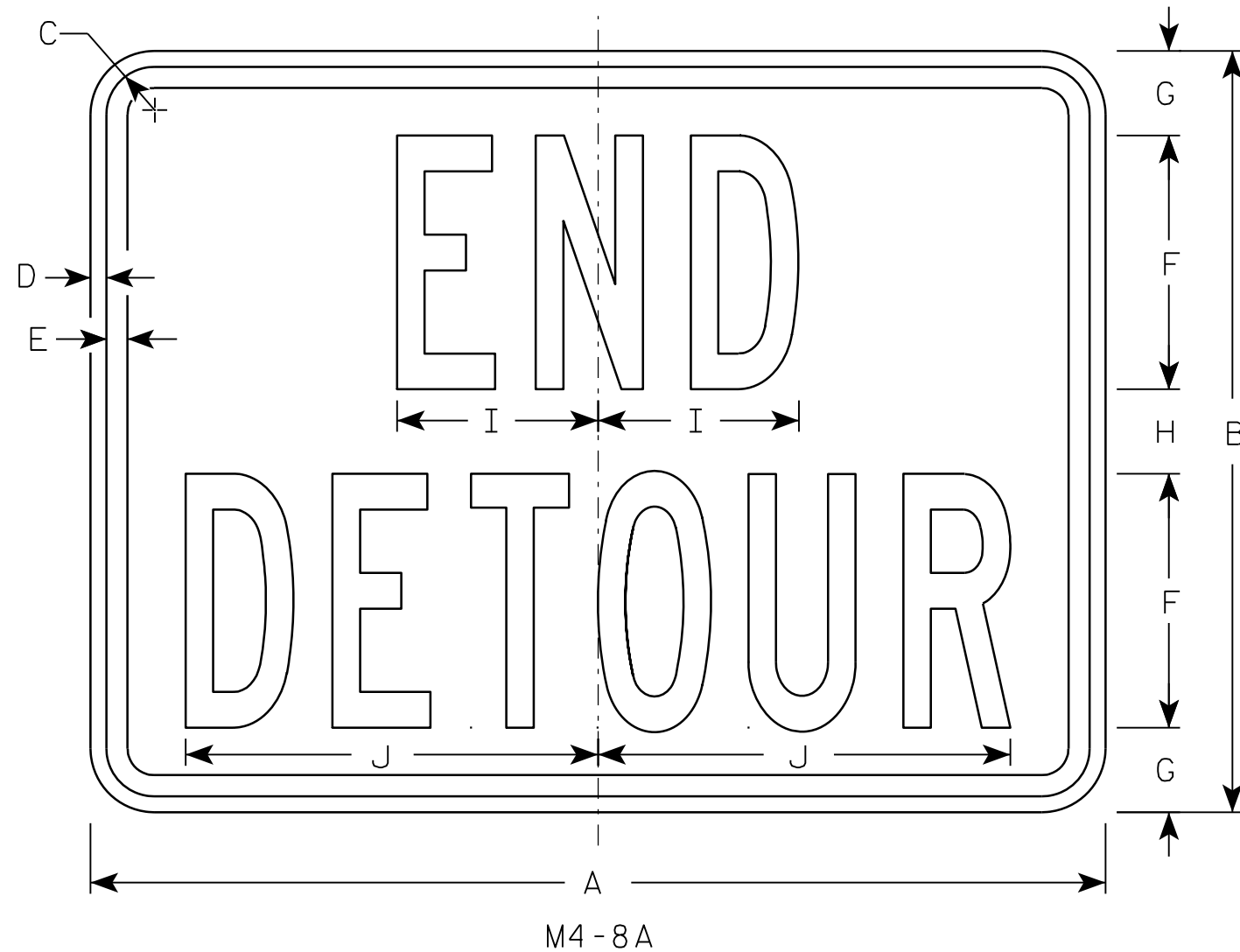
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 11/10/10 PLATE NO. M4-8.2

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - B
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	18	1 1/8	3/8	1/2	6	2	2	4 3/4	9 3/4																	3.0
3	30	24	1 1/8	3/8	1/2	8	2 1/2	3	6 3/4	13																	5.0
4																											
5																											

STANDARD SIGN
M4-8A

WISCONSIN DEPT OF TRANSPORTATION

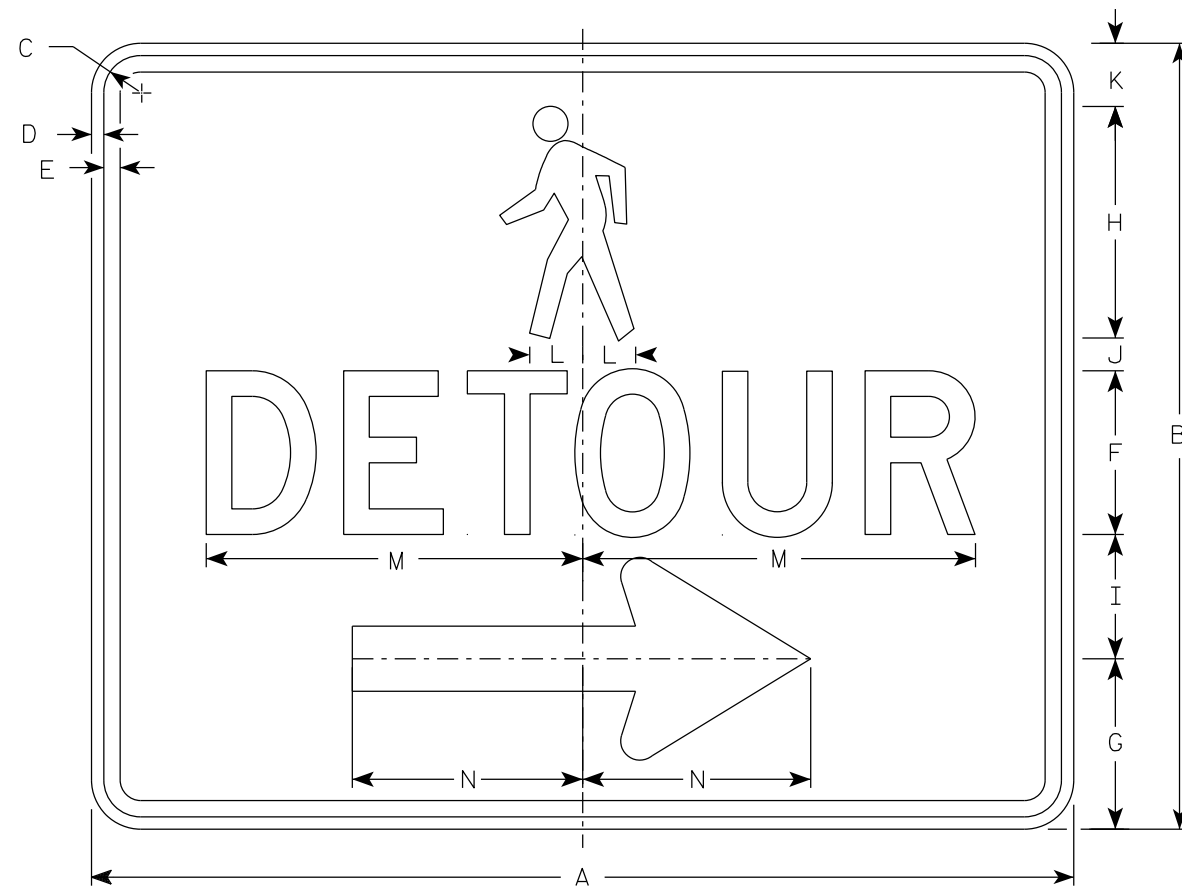
APPROVED *Matthew R Rauch*
for State Traffic Engineer

DATE 3/9/11 PLATE NO. M4-8A.2

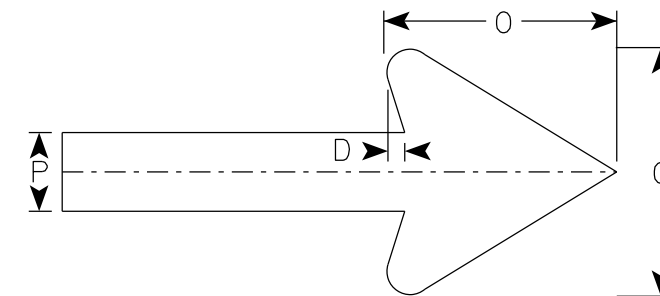
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II-Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. M4-9BL is the same as M4-9BR except the arrow is reversed.



M4 - 9BR



Arrow Detail

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	30	24	1 1/8	3/8	1/2	5	5 1/4	7 1/8	3 3/4	1	1 1/8	1 5/8	11 3/4	7	6	2											5.00
3																											
4																											
5																											

STANDARD SIGN
M4-9B L&R

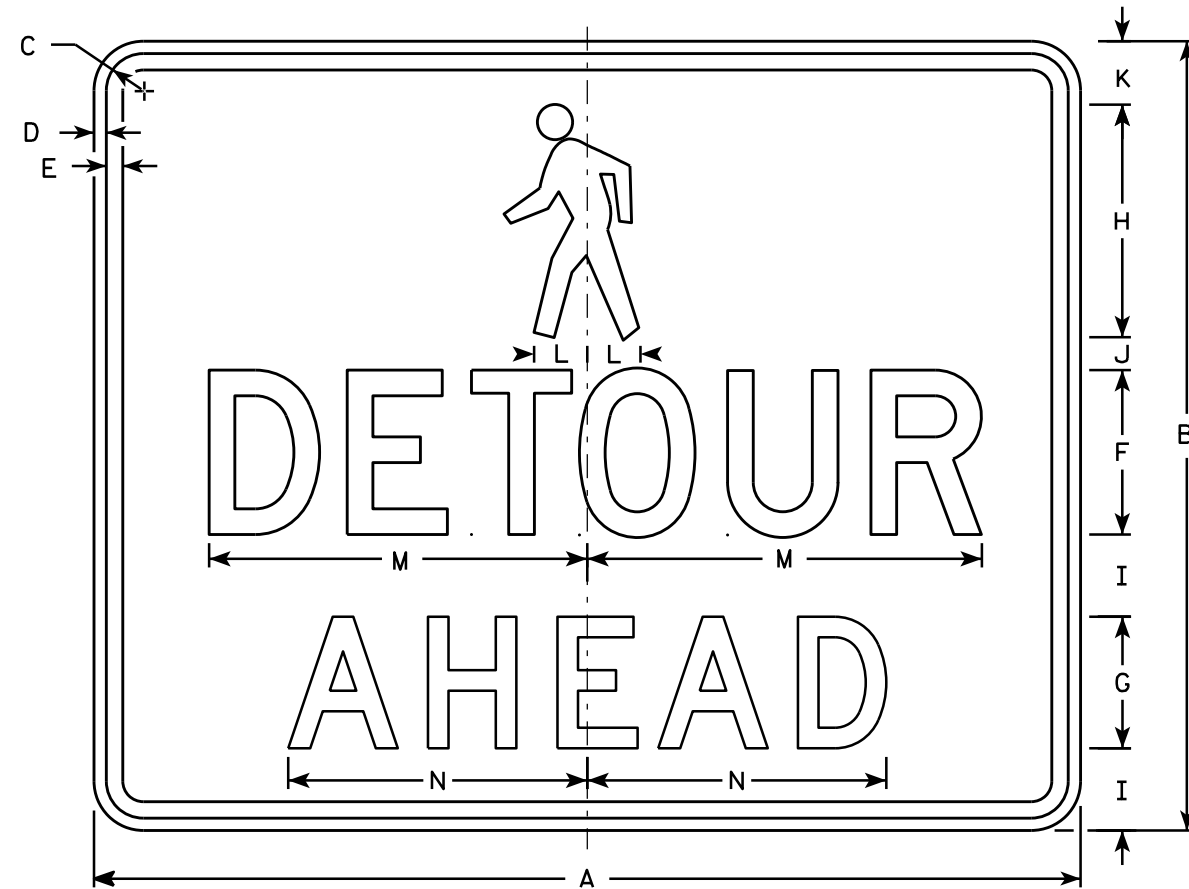
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 7/1/19 PLATE NO. M4-9B.2

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



M4-9BA

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	30	24	1 1/8	3/8	1/2	5	4	7 1/8	2 1/2	1	1 7/8	1 5/8	11 3/4	9 1/8												5.0	
3																											
4																											
5																											

STANDARD SIGN
M4-9BA

WISCONSIN DEPT OF TRANSPORTATION

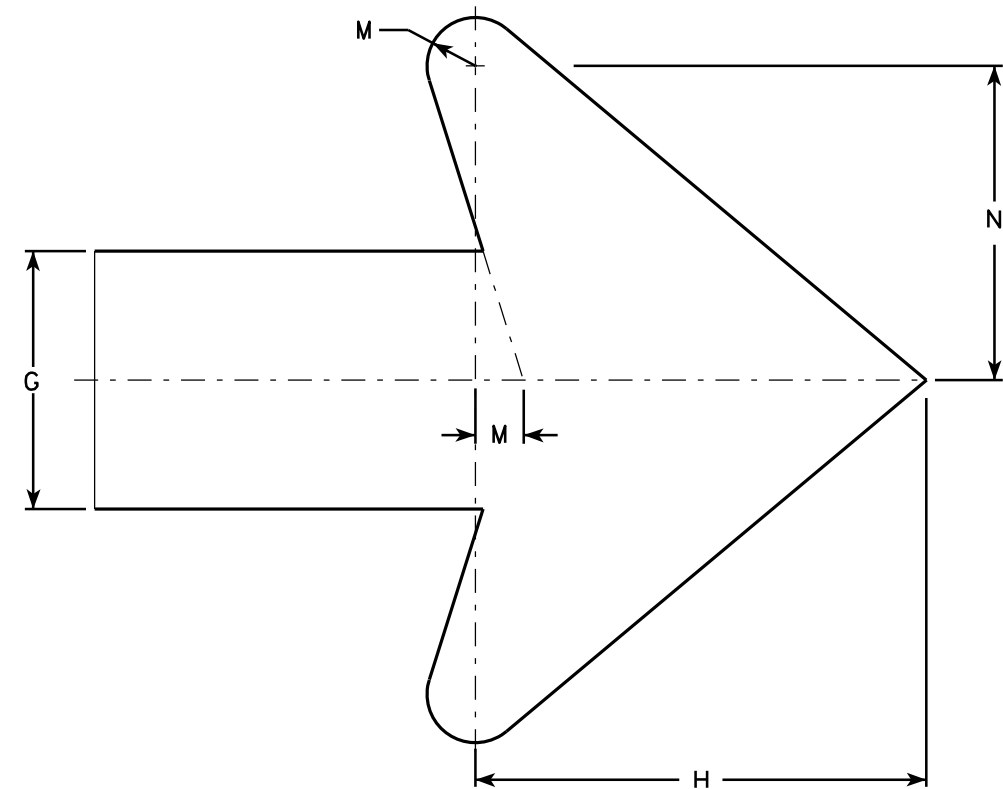
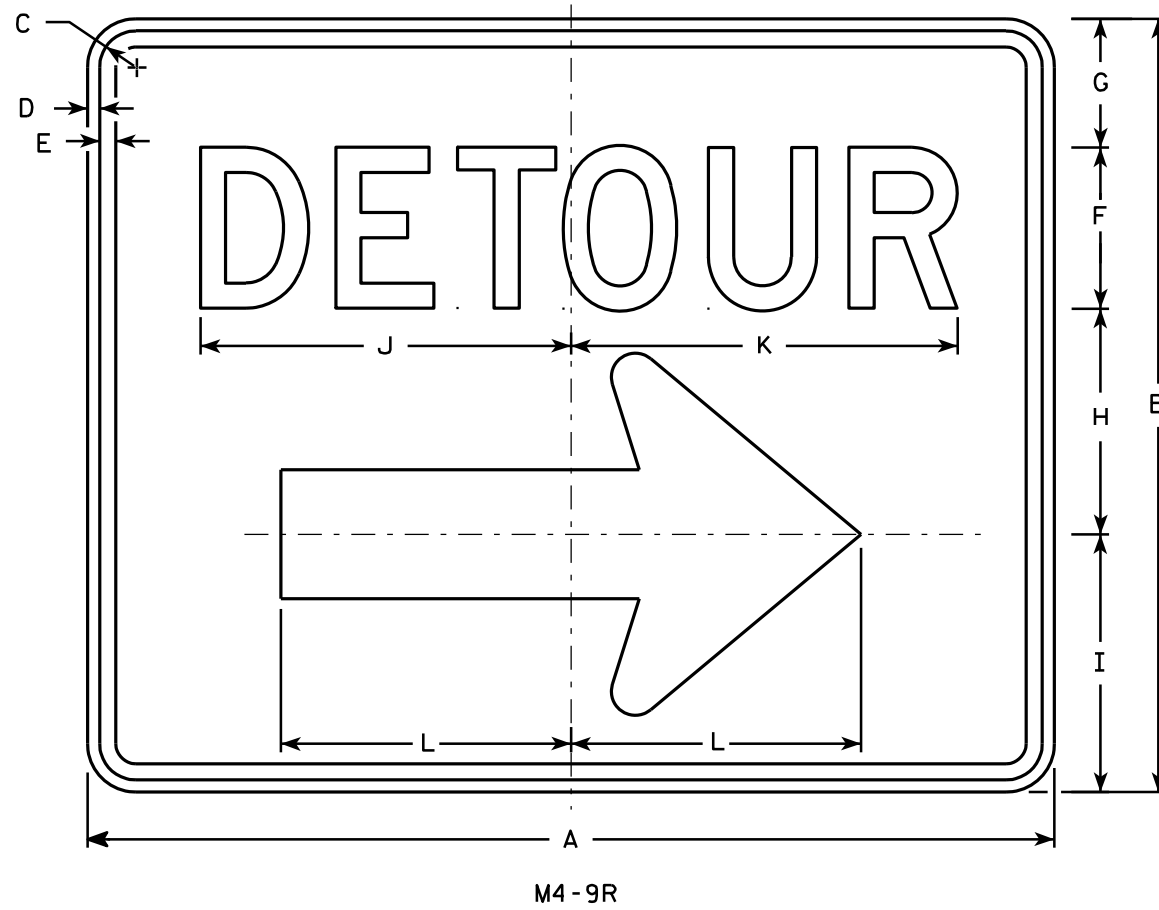
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 3/24/16 PLATE NO. M4-9BA.1

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. M4-9L is the same as M4-9R except the arrow is reversed.



Arrow Detail

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	30	24	1 1/8	3/8	1/2	5	4	7	8	11 1/2	12	9	3/4	4 7/8													5.00
3	30	24	1 1/8	3/8	1/2	5	4	7	8	11 1/2	12	9	3/4	4 7/8													5.00
4	48	36	1 3/8	1/2	5/8	8	6	10 1/2	11 5/8	20 5/8	20 1/2	13 1/4	1 1/8	6 7/8													12.0
5	48	36	1 3/8	1/2	5/8	8	6	10 1/2	11 5/8	20 5/8	20 1/2	13 1/4	1 1/8	6 7/8													12.0

STANDARD SIGN
M4-9 R & L

WISCONSIN DEPT OF TRANSPORTATION

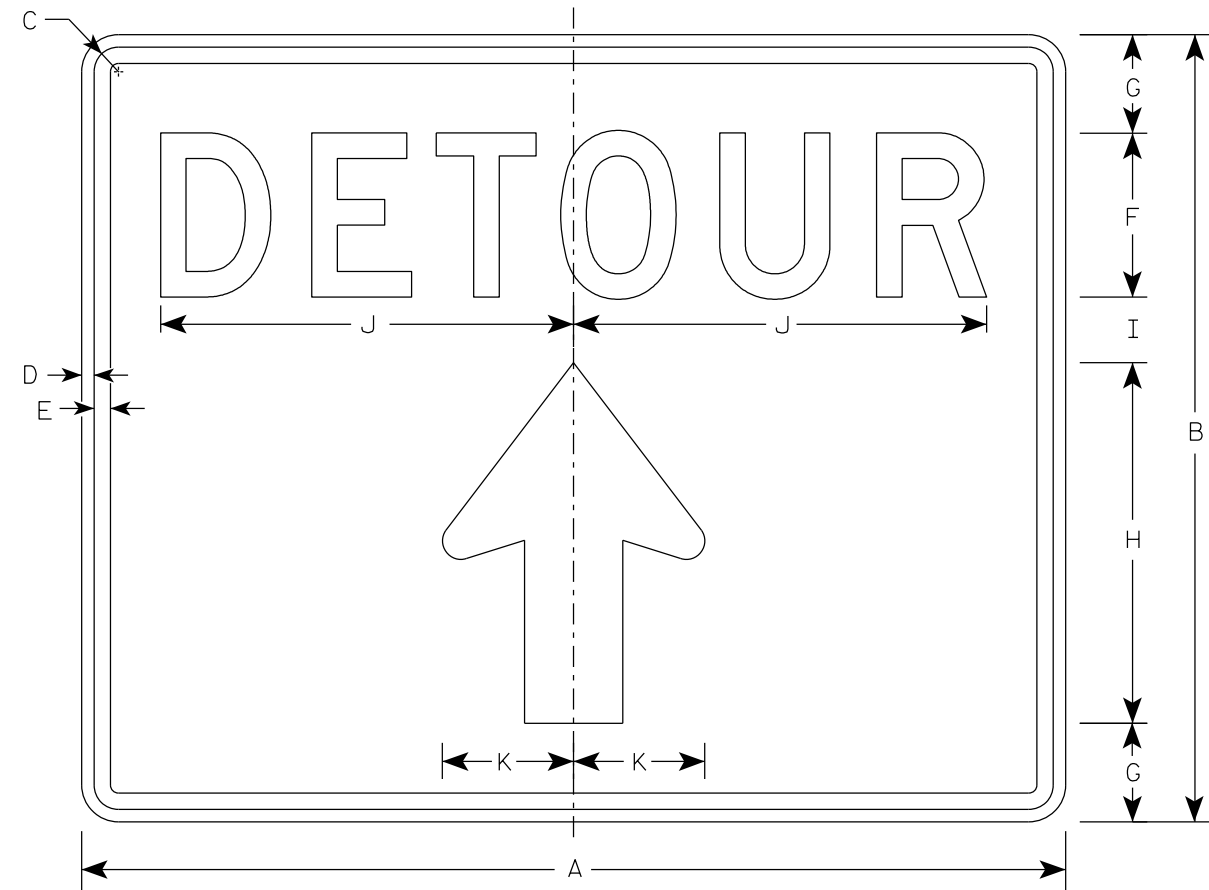
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 3/9/11 PLATE NO. M4-9R.4

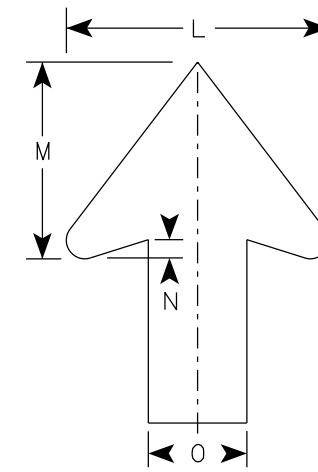
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II-Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - D



M4-9RA



Arrow Detail

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	30	24	1 1/8	3/8	1/2	5	3	11	2	12 5/8	4	8	6	1/2	3												5.00
3																											
4																											
5																											

STANDARD SIGN
M4-9RA

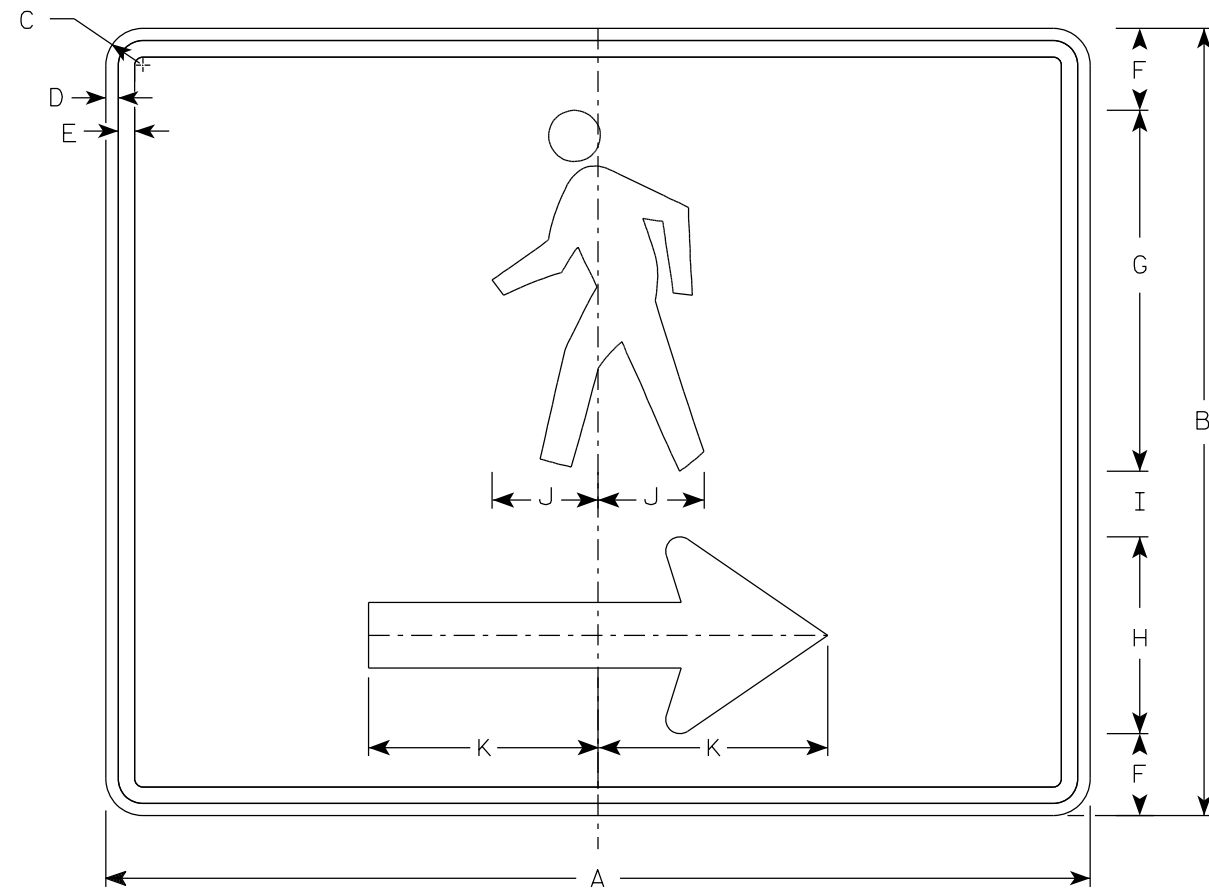
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Rauch*
For State Traffic Engineer

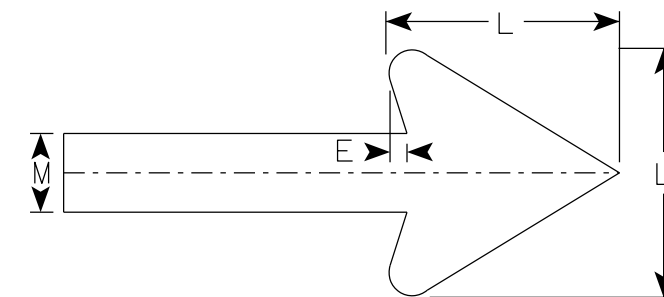
DATE 12/10/2020 PLATE NO. M4-9RA.1

NOTES

1. Sign is Type II- Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. M4-60L is the same as M4-60R except the arrow is reversed.



M4-60R



Arrow Detail

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	30	24	1 1/8	3/8	1/2	2 1/2	11	6	2	3 1/4	7	6	2														5.00
3																											
4																											
5																											

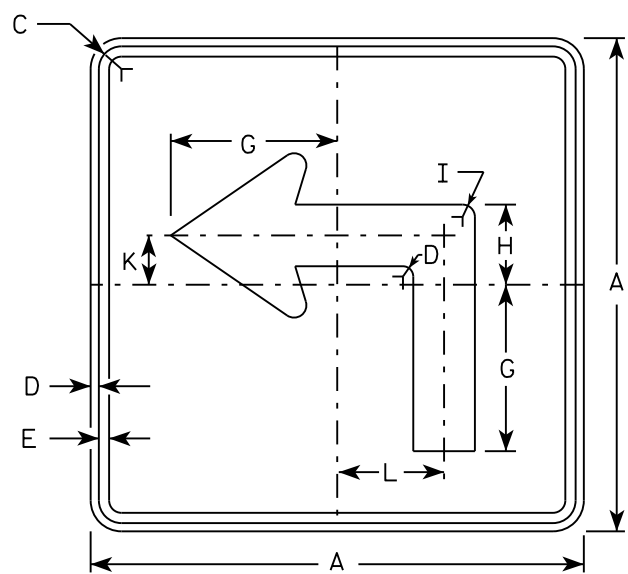
STANDARD SIGN
M4-60 L&R

WISCONSIN DEPT OF TRANSPORTATION

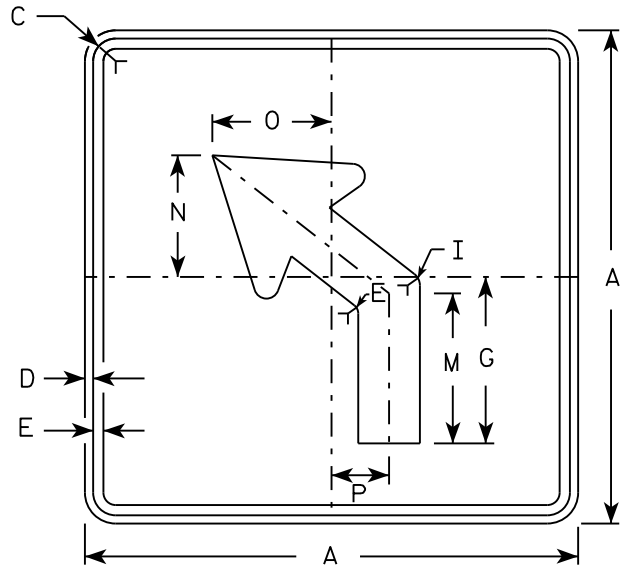
APPROVED *Matthew R Rauch*
For State Traffic Engineer

DATE 9/16/2021 PLATE NO. M4-60.1

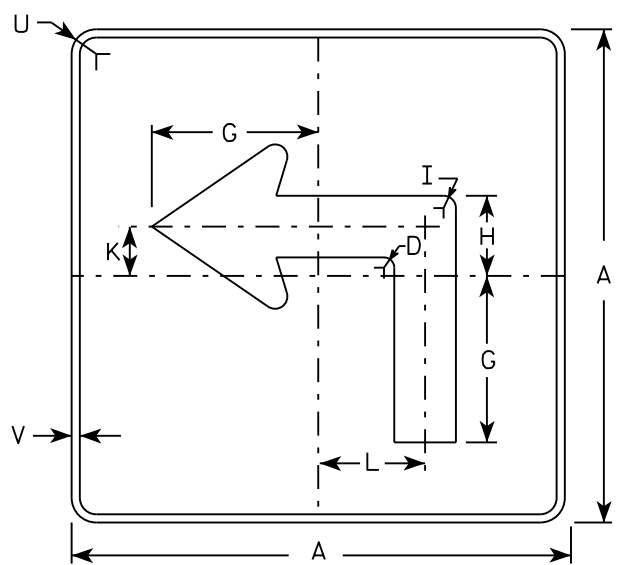
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**



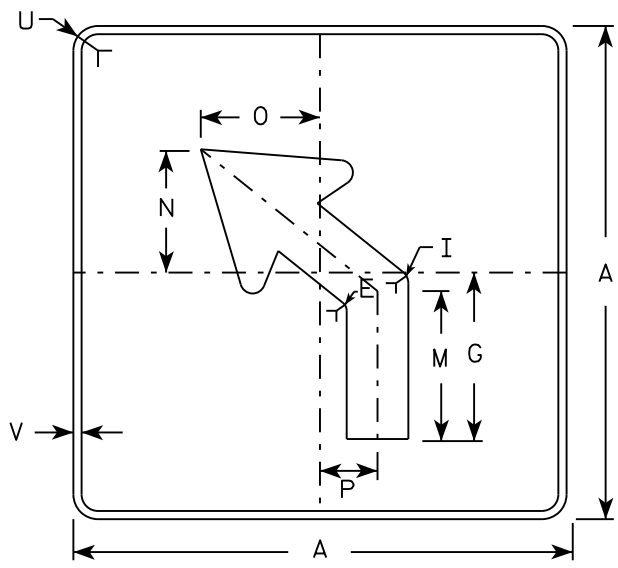
M5-1L
MM5-1L
M05-1L
MP5-1L



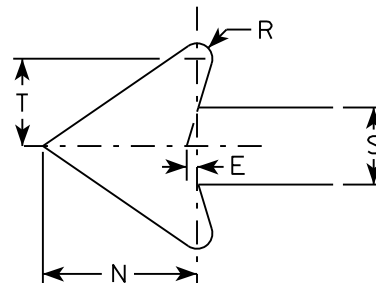
M5-2L
MM5-2L
M05-2L
MP5-2L



MB5-1L
MK5-1L
MN5-1L
MR5-1L



MB5-2L
MK5-2L
MN5-2L
MR5-2L



NOTES

- Signs are Type II - Type H reflective except as shown
- Color:
 - Background - See note 4
 - Message - See note 4
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- M5-1 and M5-2 Background - White
Message - Black
 - MB5-1 and MB5-2 Background - Blue
Message - White
 - MK5-1 and MK5-2 Background - Green
Message - White
 - MM5-1 and MM5-2 Background - White
Message - Green
 - MN5-1 and MN5-2 Background - Brown
Message - White
 - M05-1 and M05-2 Background - Orange - Type F Reflective
Message - Black
 - MP5-1 and MP5-2 Background - White - Type H Reflective
Message - Blue
 - MR5-1 and MR5-2 Background - Brown
Message - Yellow
- M5-1R same as M5-1L except arrow points right.
- M5-2R same as M5-2L except arrow tilts right.

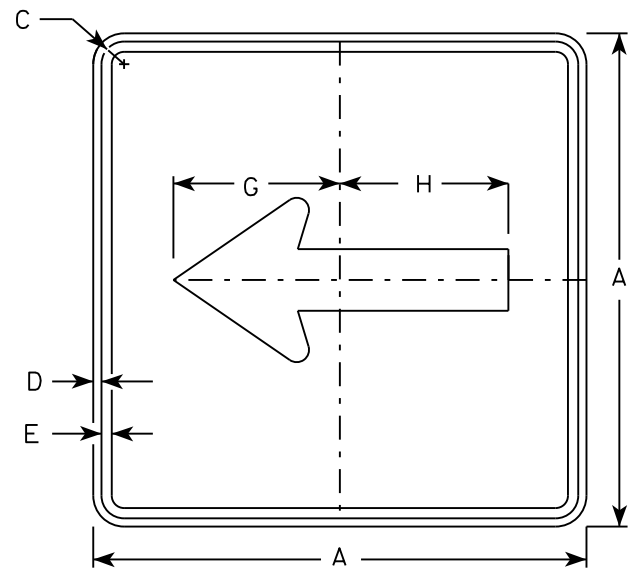
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7	3 3/8	5/8		2 1/8	4 1/2	6 3/8	5 1/4	5	2 1/2		1/2	2 5/8	3	1 1/2	1/2					3.06
3	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25
4	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25
5	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25

STANDARD SIGN
M5-1 & M5-2

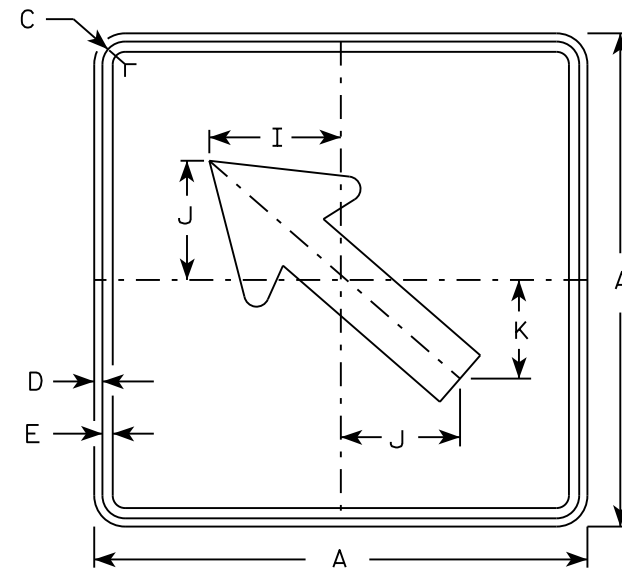
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

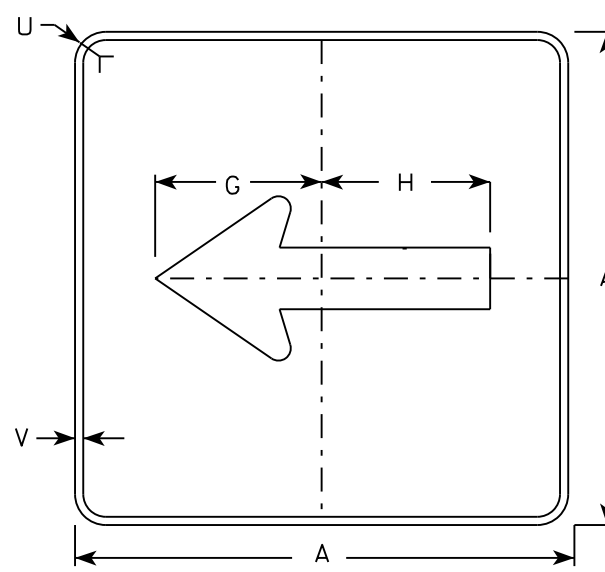
DATE 10/15/15 PLATE NO. M5-1.13



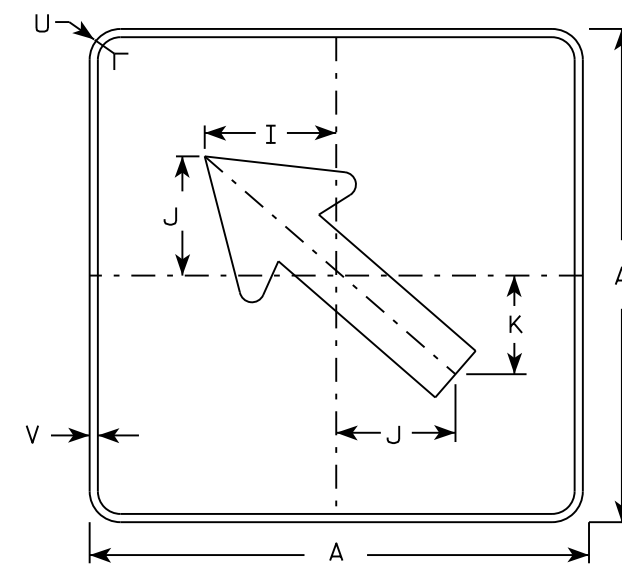
M6-1
MM6-1
M06-1
MP6-1



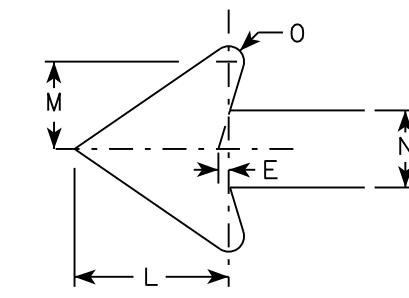
M6-2
MM6-2
M06-2
MP6-2



MB6-1
MK6-1
MN6-1
MR6-1



MB6-2
MK6-2
MN6-2
MR6-2



NOTES

- Signs are Type II - Type H except as Shown
- Color:
Background - See note 4
Message - See note 4
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- M6-1 and M6-2 Background - White
Message - Black
MB6-1 and MB6-2 Background - Blue
Message - White
MK6-1 and MK6-2 Background - Green
Message - White
MM6-1 and MM6-2 Background - White
Message - Green
MN6-1 and MN6-2 Background - Brown
Message - White
M06-1 and M06-2 Background - Orange - Type F Reflective
Message - Black
MP6-1 and MP6-2 Background - White
Message - Blue
MR6-1 and MR6-2 Background - Brown
Message - Yellow

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7 1/2	7 1/8	5 5/8	5	4 1/4	5 1/4	3	2 5/8	1/2						1 1/2	1/2					3.06
3	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25
4	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25
5	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25

STANDARD SIGN
M6-1 & M6-2
SERIES

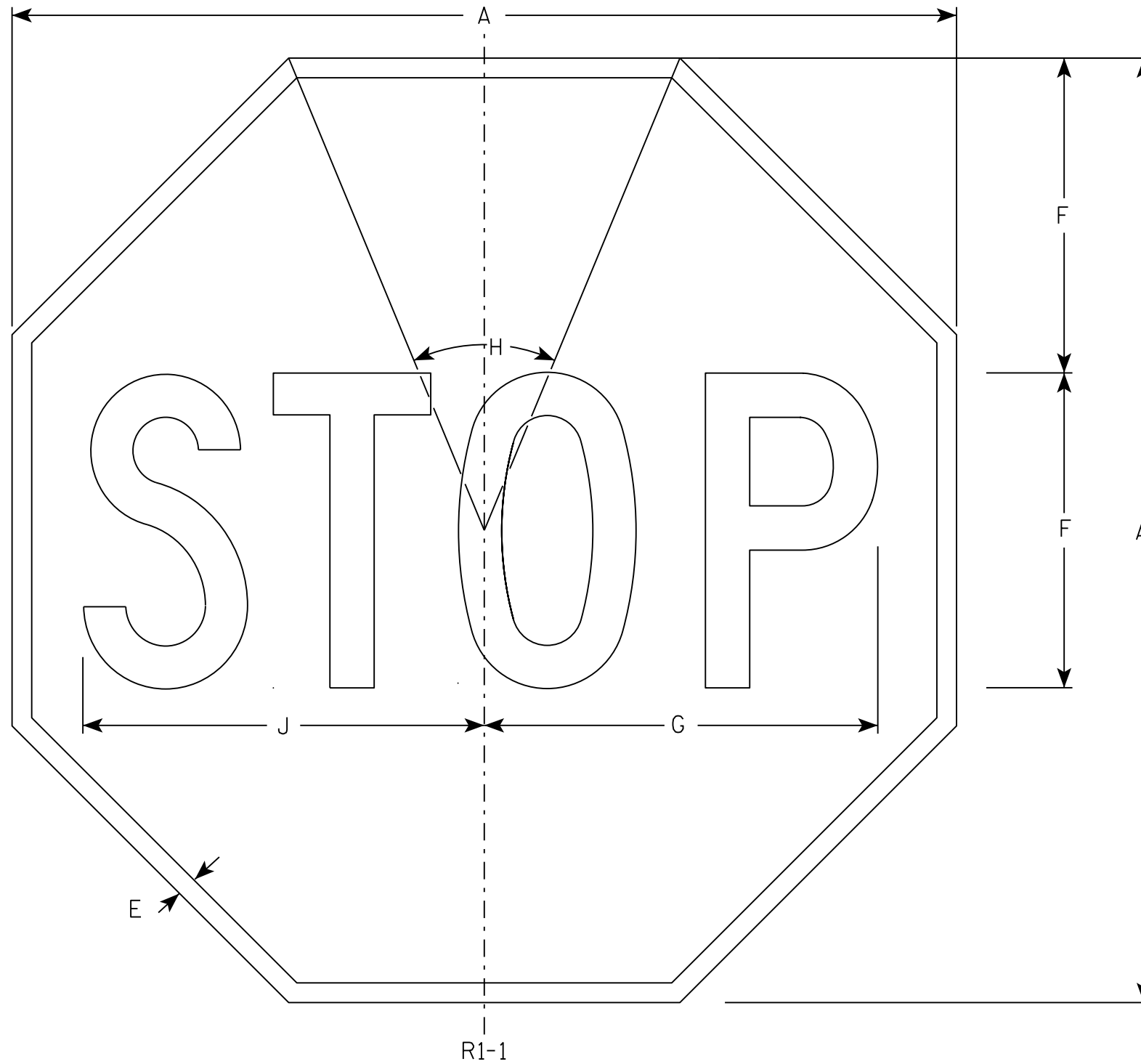
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 10/15/15 PLATE NO. M6-1.15

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Red
Message - White
3. Message Series - C



R1-1

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30				5/8	10	12 1/2	45°		12 3/4																	5.18
2S	30				5/8	10	12 1/2	45°		12 3/4																	5.18
2M	36				3/4	12	15	45°		15 3/8																	7.46
3	36				3/4	12	15	45°		15 3/8																	7.46
4	48				1	16	20	45°		20 1/2																	13.25
5	48				1	16	20	45°		20 1/2																	13.25
6	18				3/8	6	7 3/4	45°		7 3/4																	1.86
7	12				1/4	4	5	45°		5 1/8																	0.78

STANDARD SIGN
R1-1

WISCONSIN DEPT OF TRANSPORTATION

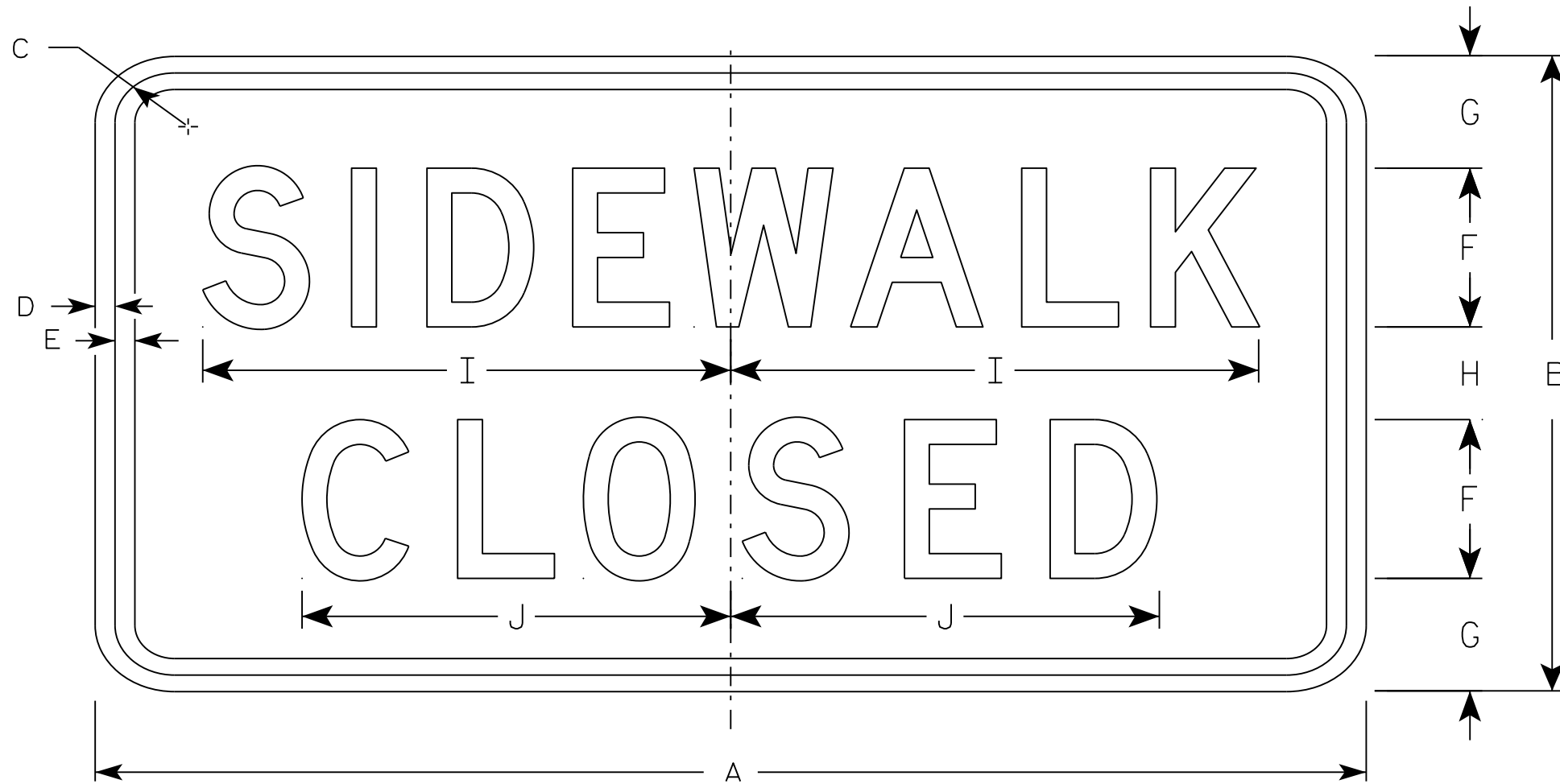
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 11/12/15 PLATE NO. R1-1.13

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Use Size 2 for Sidewalks. Use Size 3 for Paths and Trails.



R9-9

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	12	1 3/4	1/2	1/2	3	2 1/8	1 3/4	10	8 1/8																	2.0
2M	24	12	1 3/4	1/2	1/2	3	2 1/8	1 3/4	10	8 1/8																	2.0
3	30	18	1 3/4	1/2	1/2	4	3 1/2	3	12 1/2	10 1/4																	3.75
4																											
5																											

STANDARD SIGN
R9-9

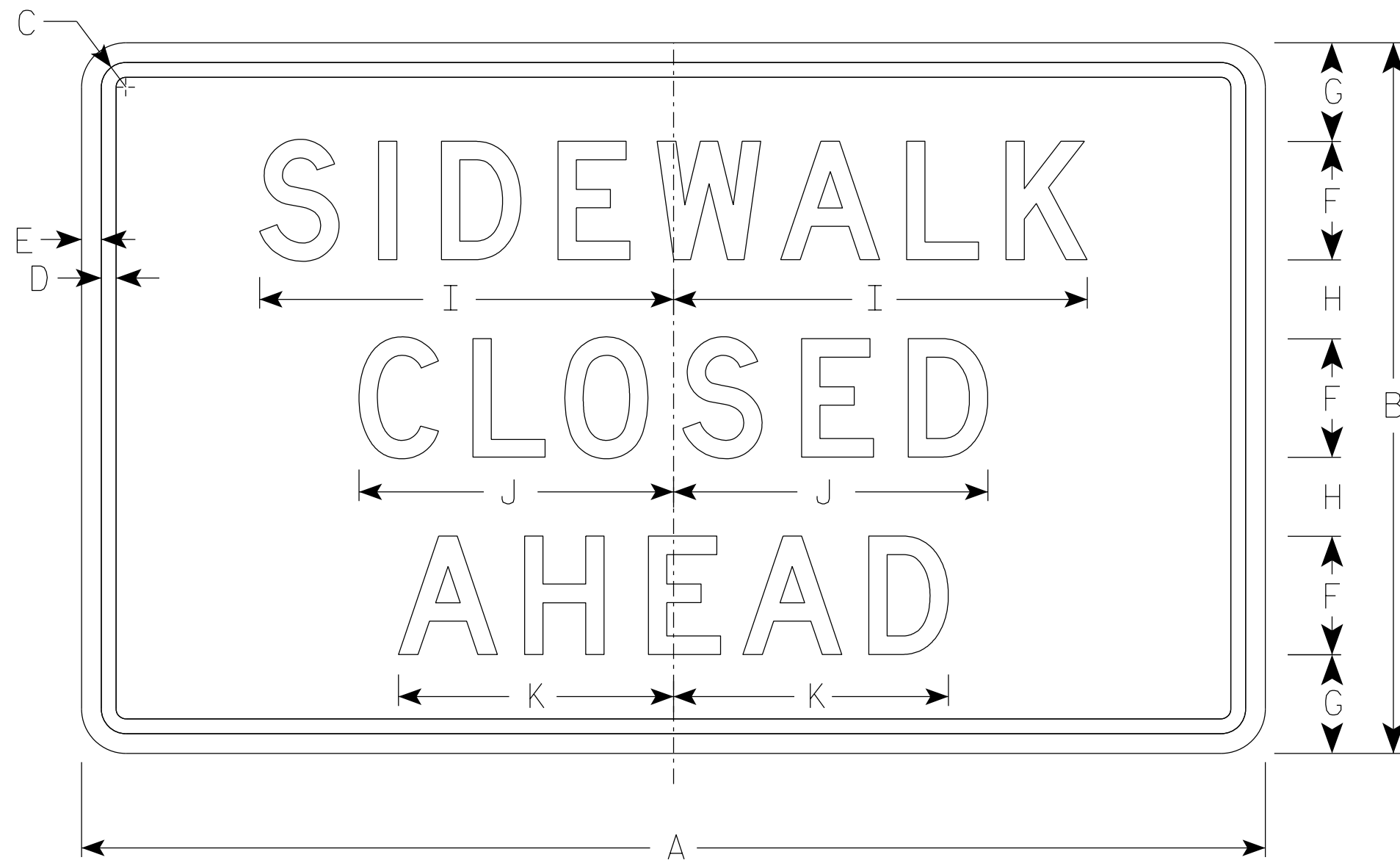
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 8/11/16 PLATE NO. R9-9.6

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



R9-9A

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	30	18	1 1/8	3/8	1/2	3	2 1/2	2	10 1/2	8	7																3.75
2M	30	18	1 1/8	3/8	1/2	3	2 1/2	2	10 1/2	8	7																3.75
3																											
4																											
5																											

STANDARD SIGN
R9-9A

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 8/31/2020 PLATE NO. R9-9A.1

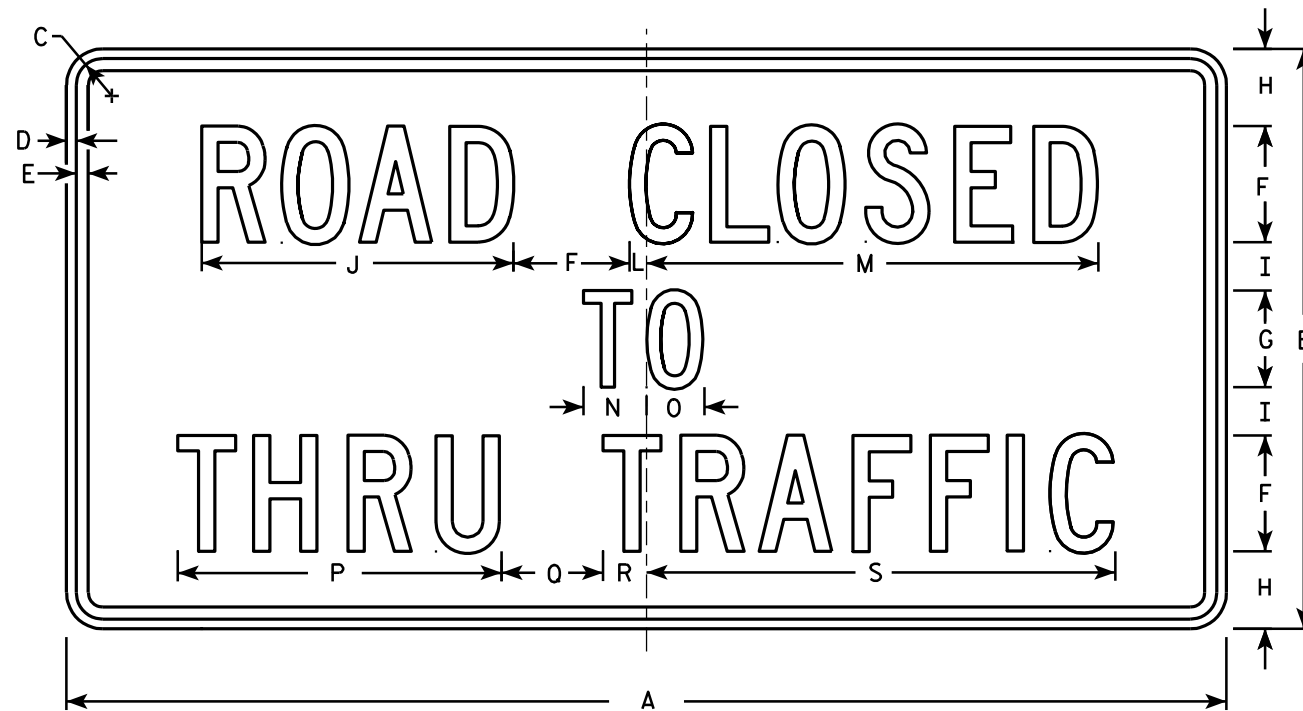
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

7

7

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



R11-4

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	60	30	1 3/8	1/2	5/8	6	5	4	2 1/2	16 1/8		7/8	23 3/8	3 1/4	3	16 3/4	5 1/4	2 1/4	24 1/4								12.5
2M	60	30	1 3/8	1/2	5/8	6	5	4	2 1/2	16 1/8		7/8	23 3/8	3 1/4	3	16 3/4	5 1/4	2 1/4	24 1/4								12.5
3																											
4																											
5																											

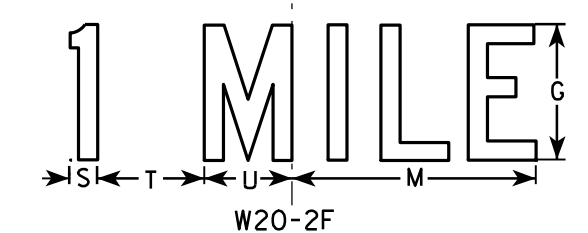
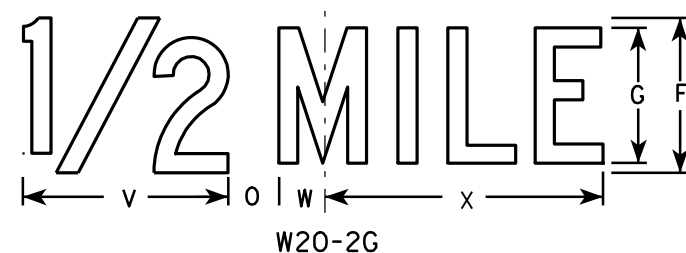
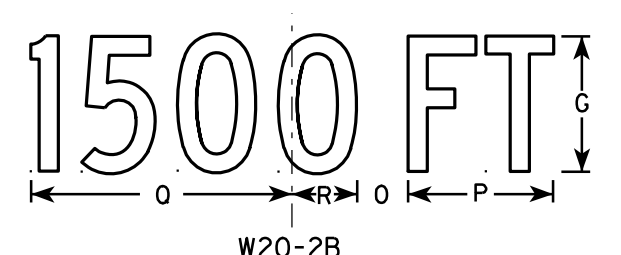
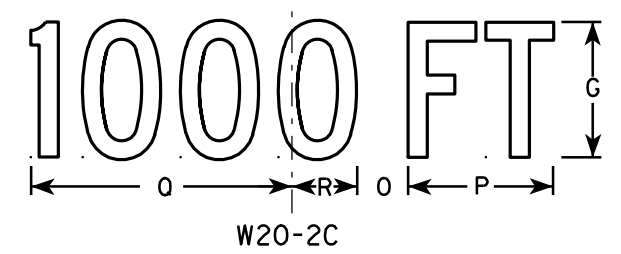
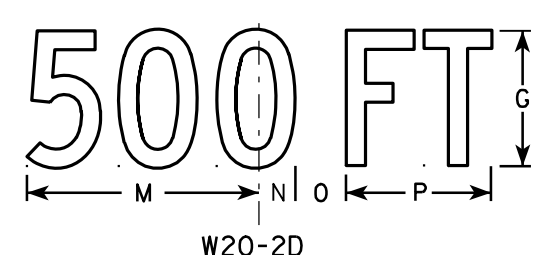
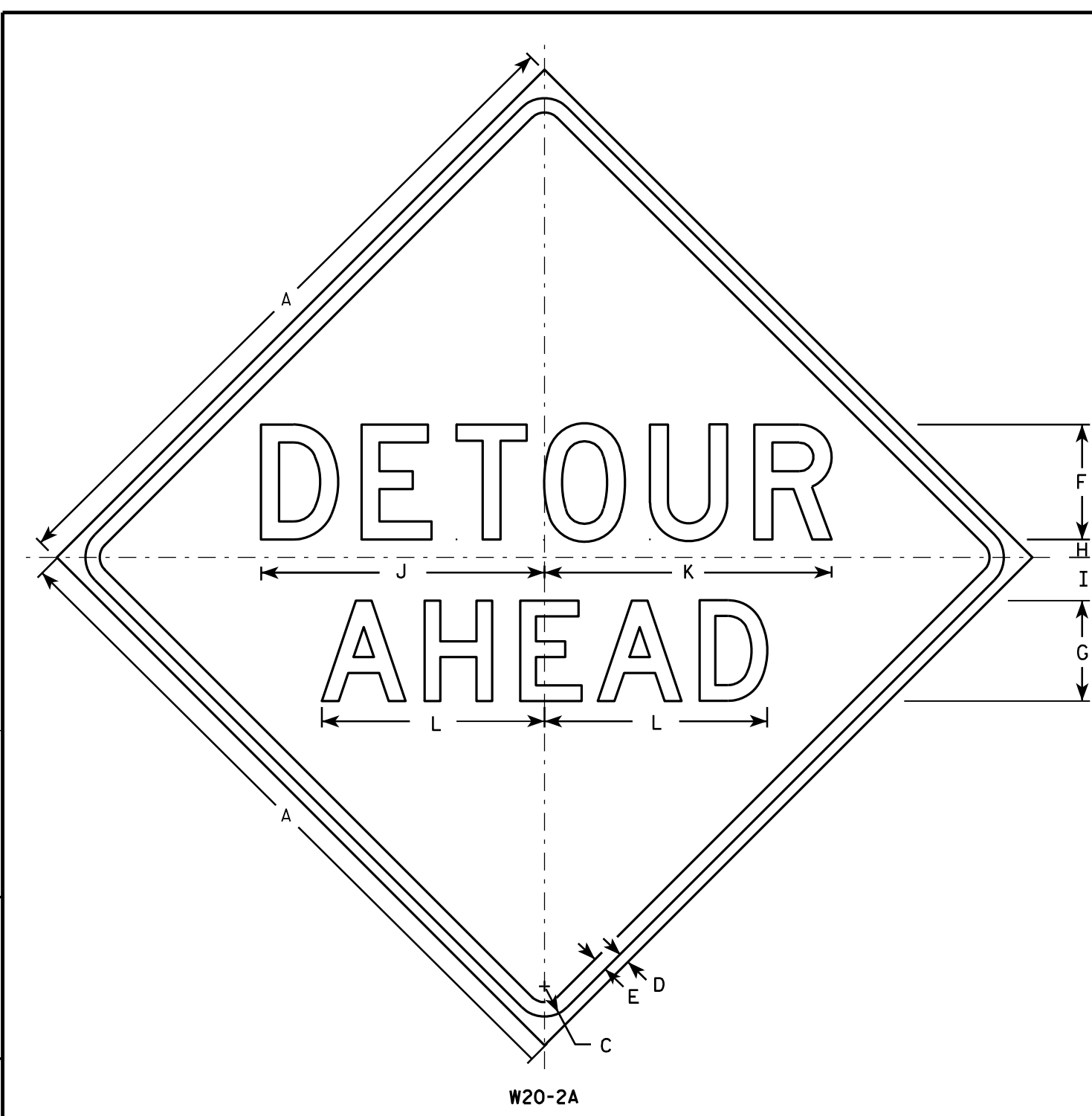
STANDARD SIGN
R11 - 4

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Raush*
for State Traffic Engineer

DATE 4/1/11 PLATE NO. R11-4.3

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - See note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Line 1 is Series D.
Line 2 is Series D for AHEAD and Series C for all other distances.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	6	5	1	2 1/4	14 3/4	15	11 5/8	9	1 3/8	1 7/8	5 5/8	10 1/8	2 1/2	1 1/8	4 1/2	3 1/2	8	1 3/4	10 3/4			9.0
2S	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
2M	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
3	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
4	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
5	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0

STANDARD SIGN
W20-2A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Raub*
for State Traffic Engineer

DATE 3/18/11 PLATE NO. W20-2.6

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E

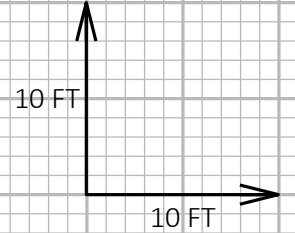
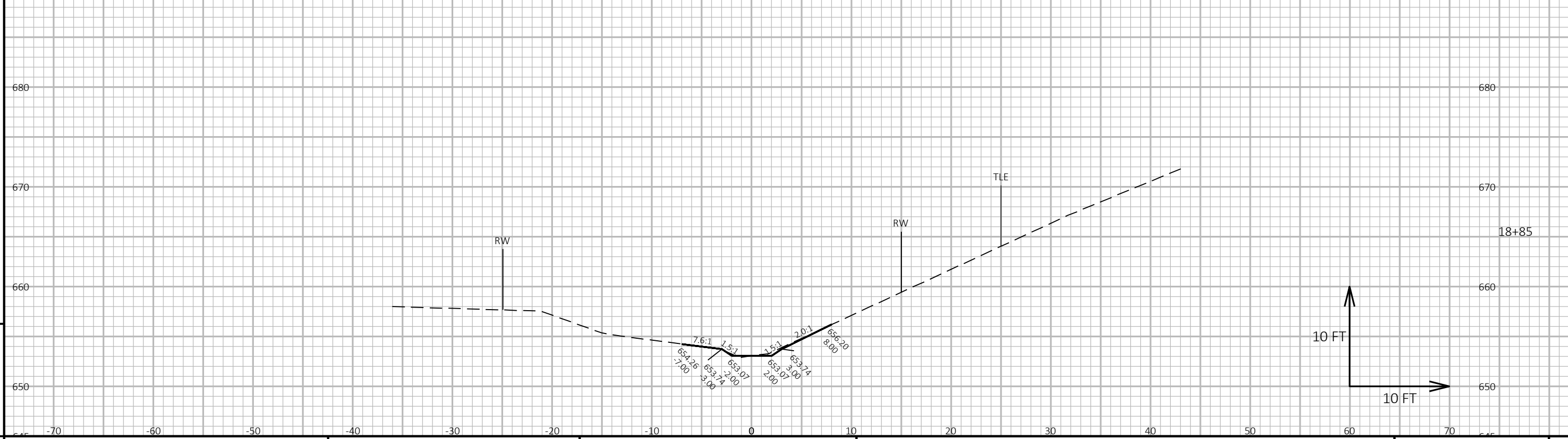
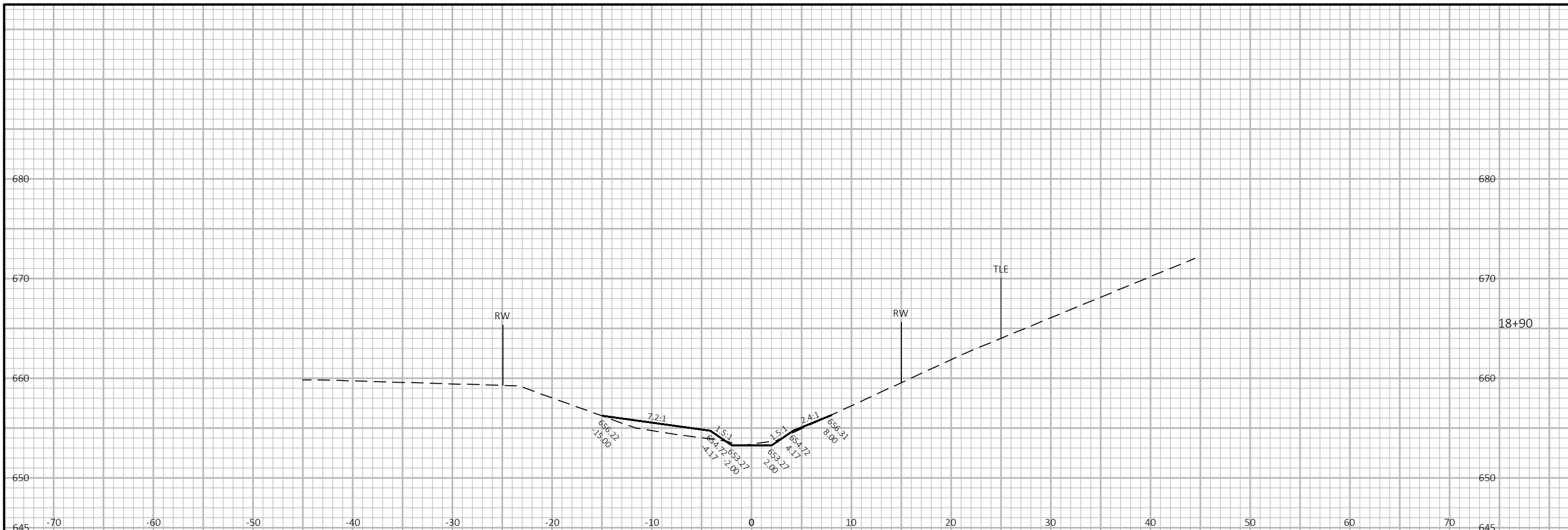
EARTHWORK-STH 96 CULVERT DITCH STA. 164+00 RT

STATION	AREA (SF)		INCREMENTAL VOL (CY) (UNADJUSTED)		CUMULATIVE VOL (CY)		
	CUT	FILL	CUT	FILL	CUT	EXPANDED FILL	MASS ORDINATE
					1.00	1.30	
18+85	1.11	0.12	0.0	0.0	0	0	0
18+90	0.92	8.31	0.2	0.8	0	1	-1
19+00	0.78	17.62	0.3	4.8	1	7	-7
19+10	0.35	25.94	0.2	8.1	1	18	-17
19+20	2.98	36.22	0.6	11.5	1	33	-31
19+30	3.29	58.78	1.2	17.6	3	56	-53
19+40	0.87	58.73	0.8	21.8	3	84	-81
19+50	7.35	19.98	1.5	14.6	5	103	-98
19+60	24.29	3.46	5.9	4.3	11	109	-98
19+70	26.97	2.60	9.5	1.1	20	110	-90
19+80	24.49	2.93	9.5	1.0	30	111	-82
19+90	17.39	23.01	7.8	4.8	38	118	-80
19+95	13.60	0.00	2.9	2.1	40	120	-80

40 90

NOTES:

NOTE 8 - MASS ORDINATE = CUT - (FILL * FILL FACTOR)



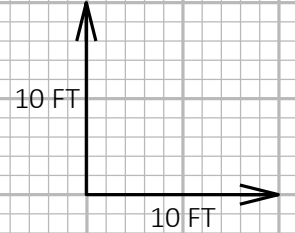
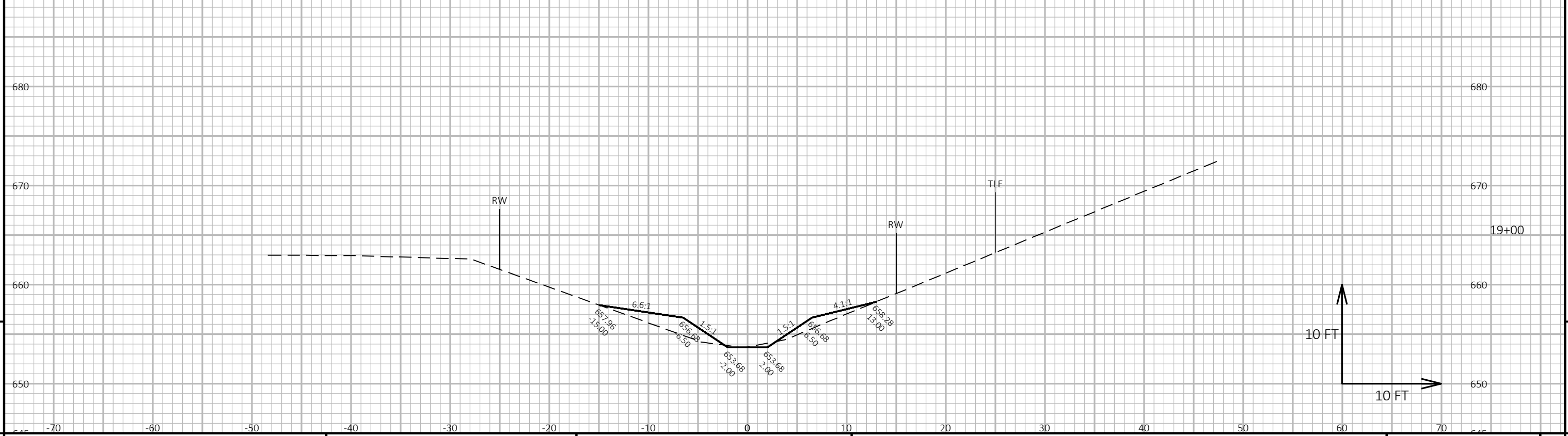
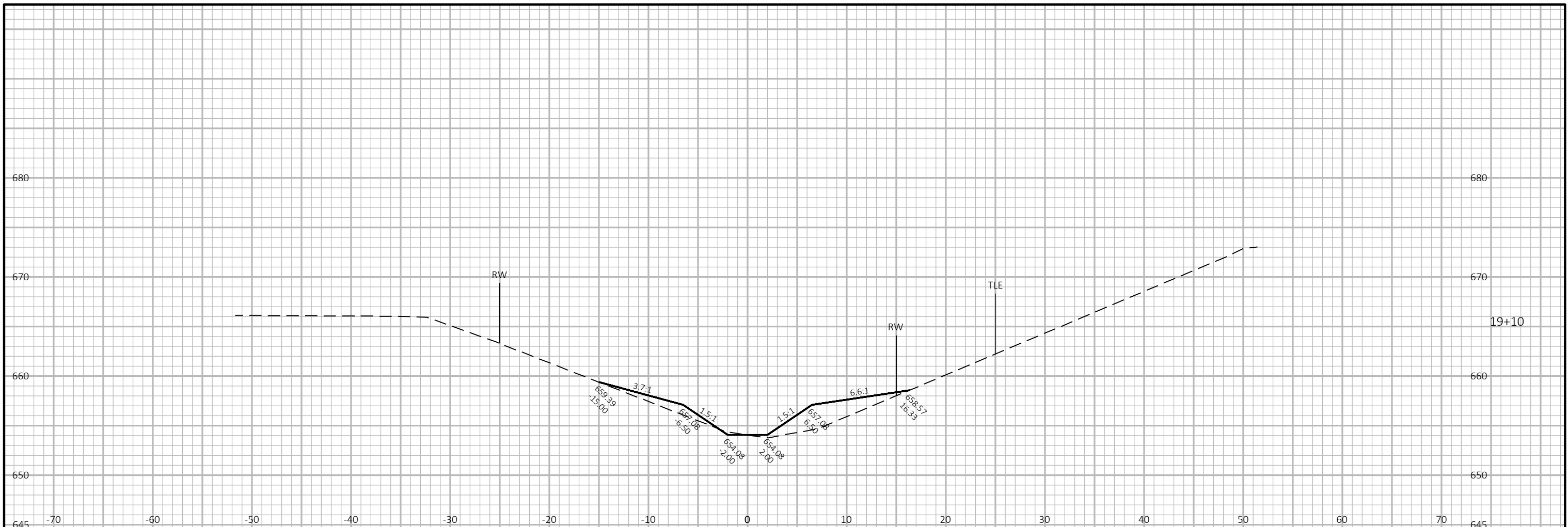
9

9

PROJECT NO: 4075-35-71/72 HWY: STH 96 COUNTY: BROWN & OUTAGAMIE CROSS SECTIONS: DITCH SHEET E

FILE NAME : I:\45\450532 STH 96 OUTAGAMIE CO\C3D\SHEETSPLAN\090201-XS.DWG PLOT DATE : 7/21/2023 1:02 PM PLOT BY : ROWELL, ANDREW PLOT NAME : PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49

LAYOUT NAME - 090201-xs



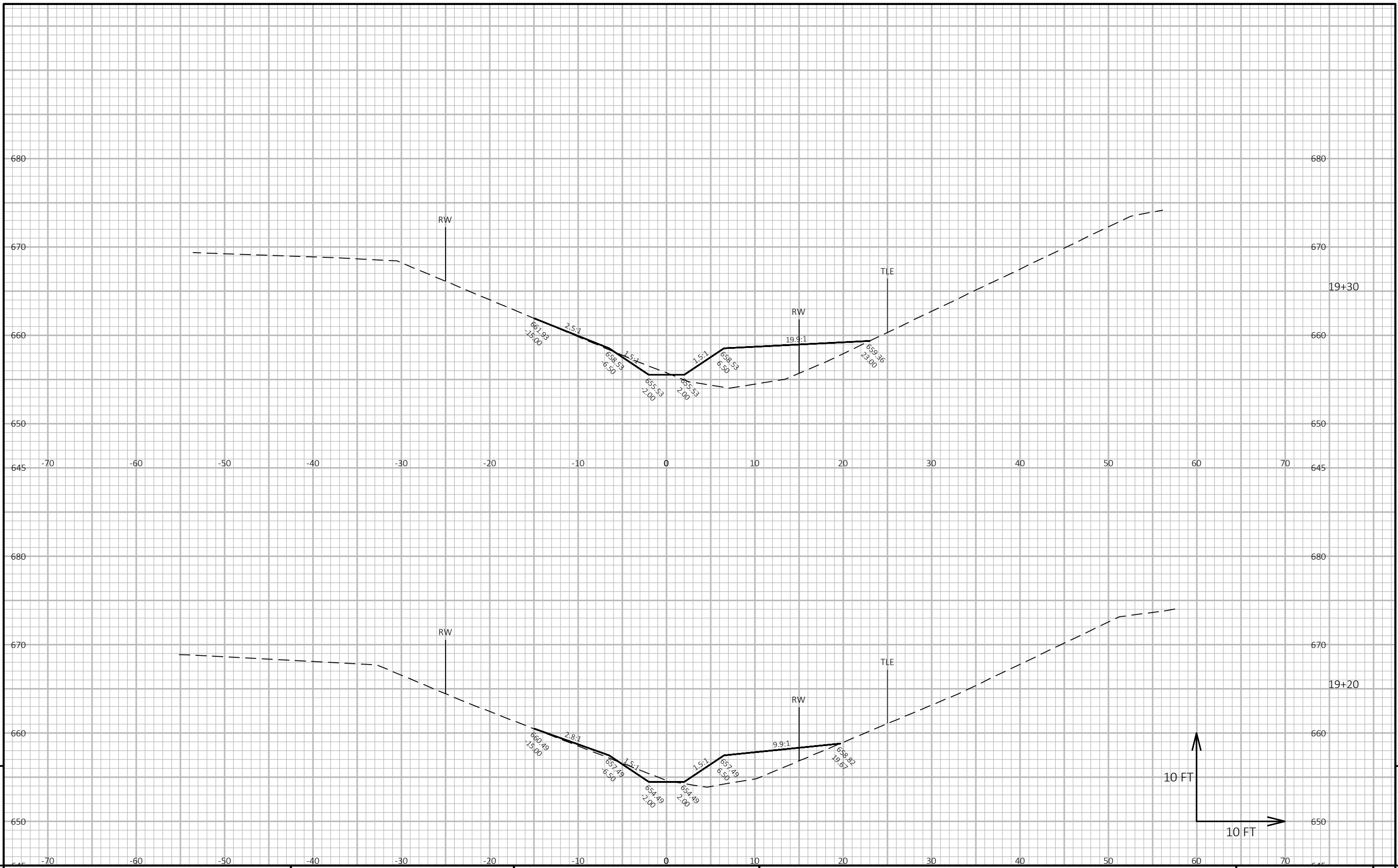
9

9

PROJECT NO: 4075-35-71/72	HWY: STH 96	COUNTY: BROWN & OUTAGAMIE	CROSS SECTIONS: DITCH	SHEET	E
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FILE NAME : I:\45\450532 STH 96 OUTAGAMIE CO\C3D\SHEETSPLAN\090201-XS.DWG PLOT DATE : 7/21/2023 1:02 PM PLOT BY : ROWELL, ANDREW PLOT NAME : PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49

LAYOUT NAME - 090202-xs



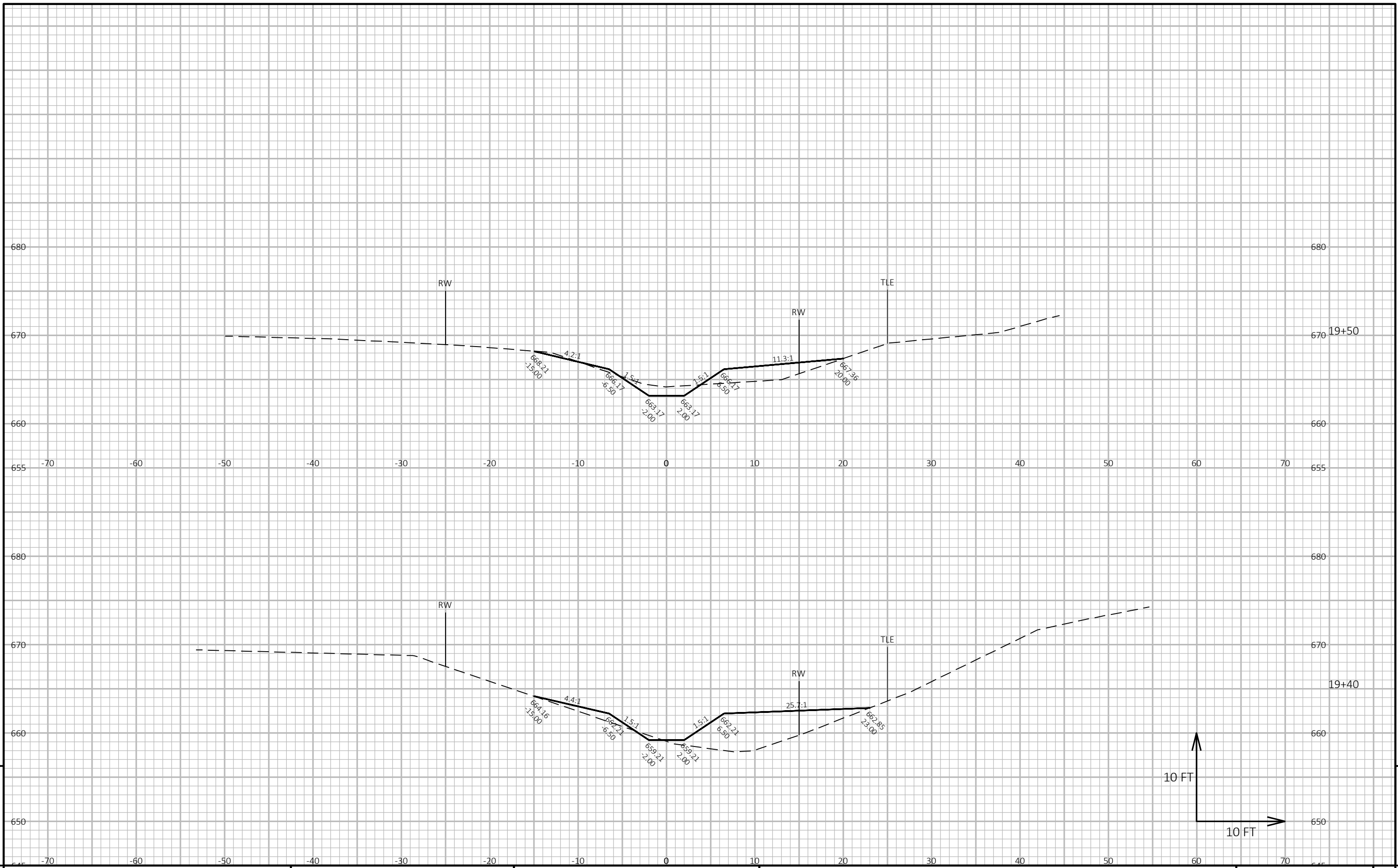
9

9

PROJECT NO: 4075-35-71/72 HWY: STH 96 COUNTY: BROWN & OUTAGAMIE CROSS SECTIONS: DITCH SHEET E

FILE NAME : I:\45\450532 STH 96 OUTAGAMIE CO\C3D\SHEETSPLAN\090201-XS.DWG PLOT DATE : 7/21/2023 1:02 PM PLOT BY : ROWELL, ANDREW PLOT NAME : PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49

LAYOUT NAME - 090203-xs

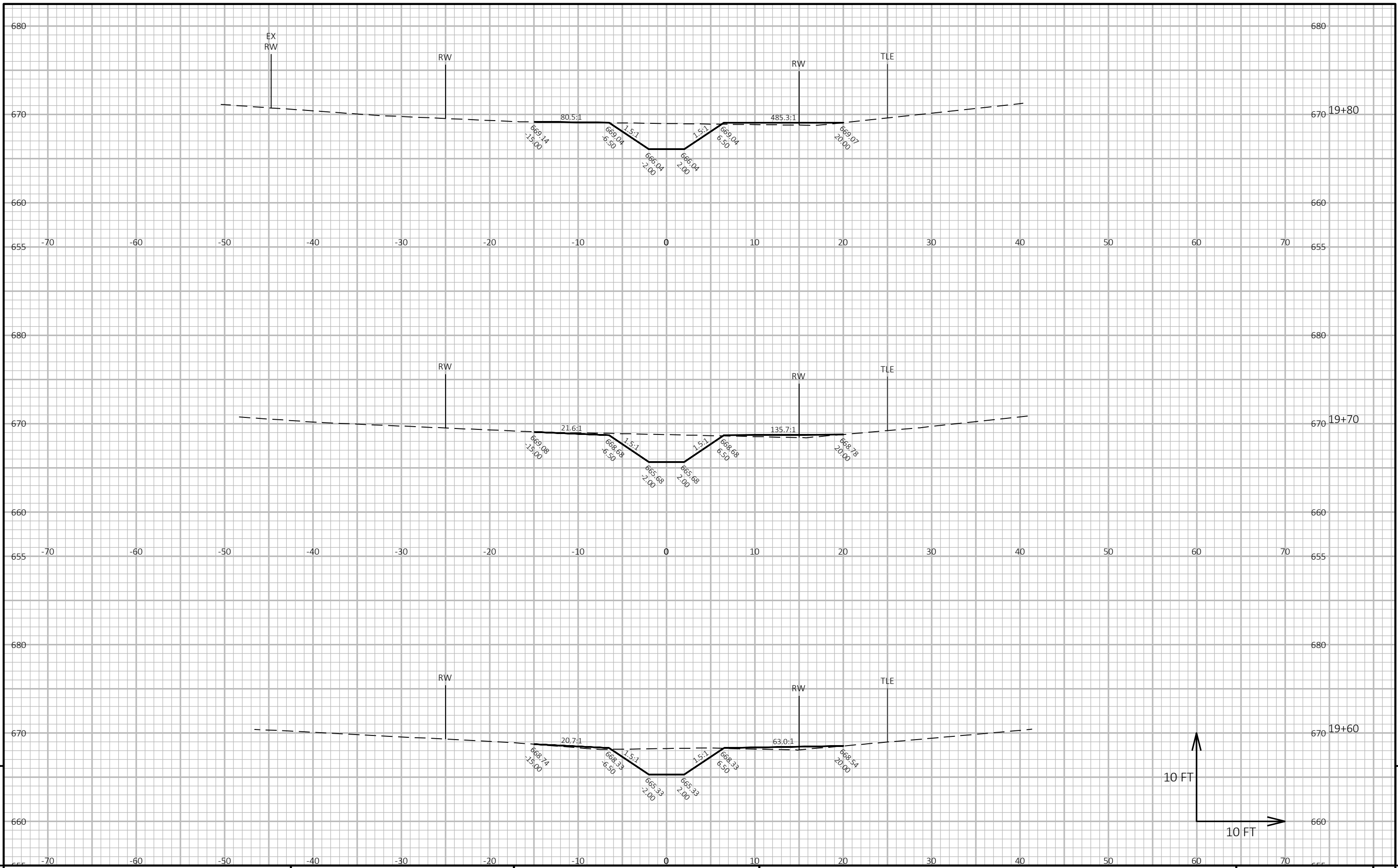


9

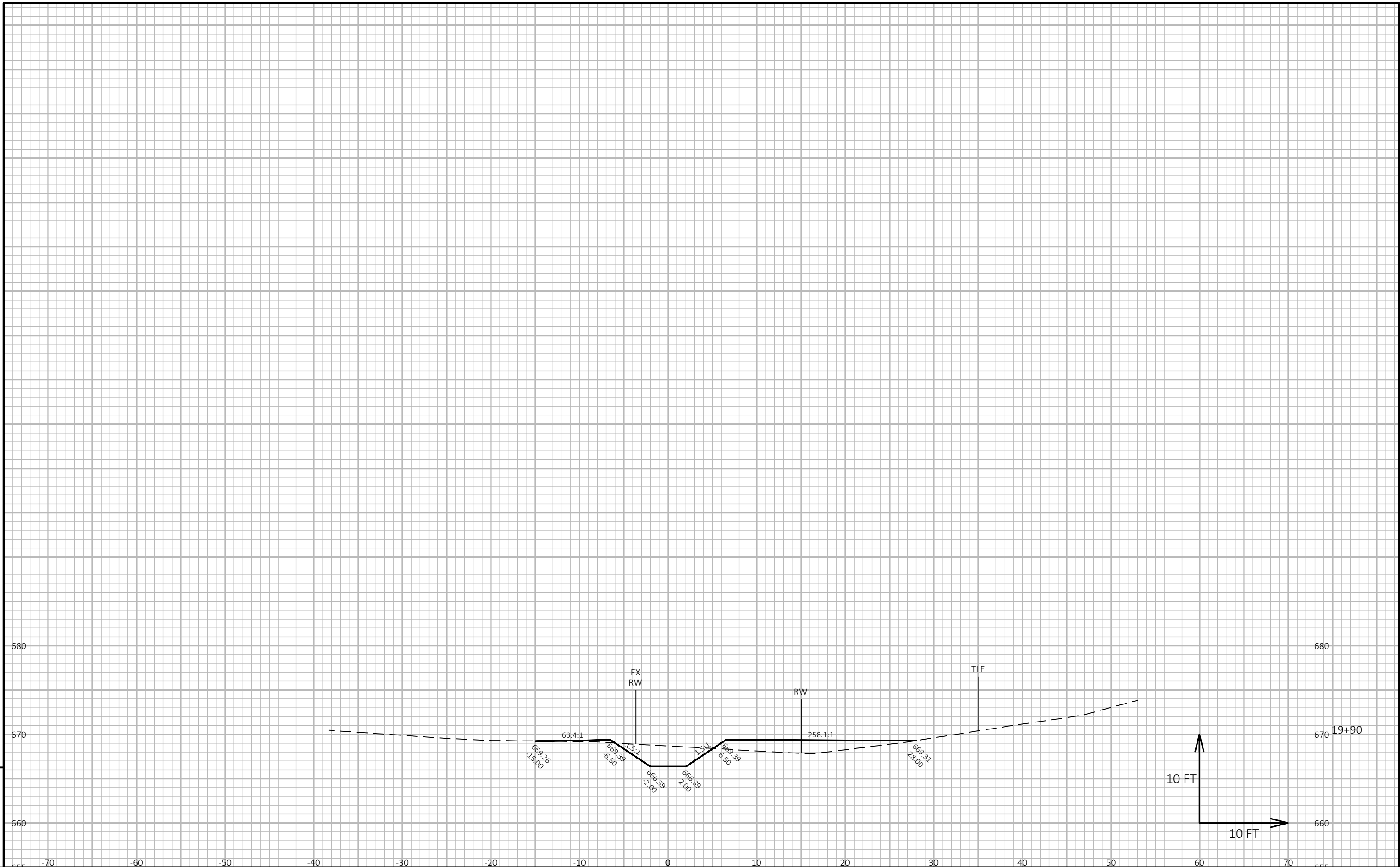
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PROJECT NO: 4075-35-71/72 HWY: STH 96 COUNTY: BROWN & OUTAGAMIE CROSS SECTIONS: DITCH SHEET E

FILE NAME: I:\45\450532 STH 96 OUTAGAMIE CO\C3D\SHEETSPLAN\090201-XS.DWG PLOT DATE: 7/21/2023 1:02 PM PLOT BY: ROWELL, ANDREW PLOT NAME: PLOT SCALE: 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49



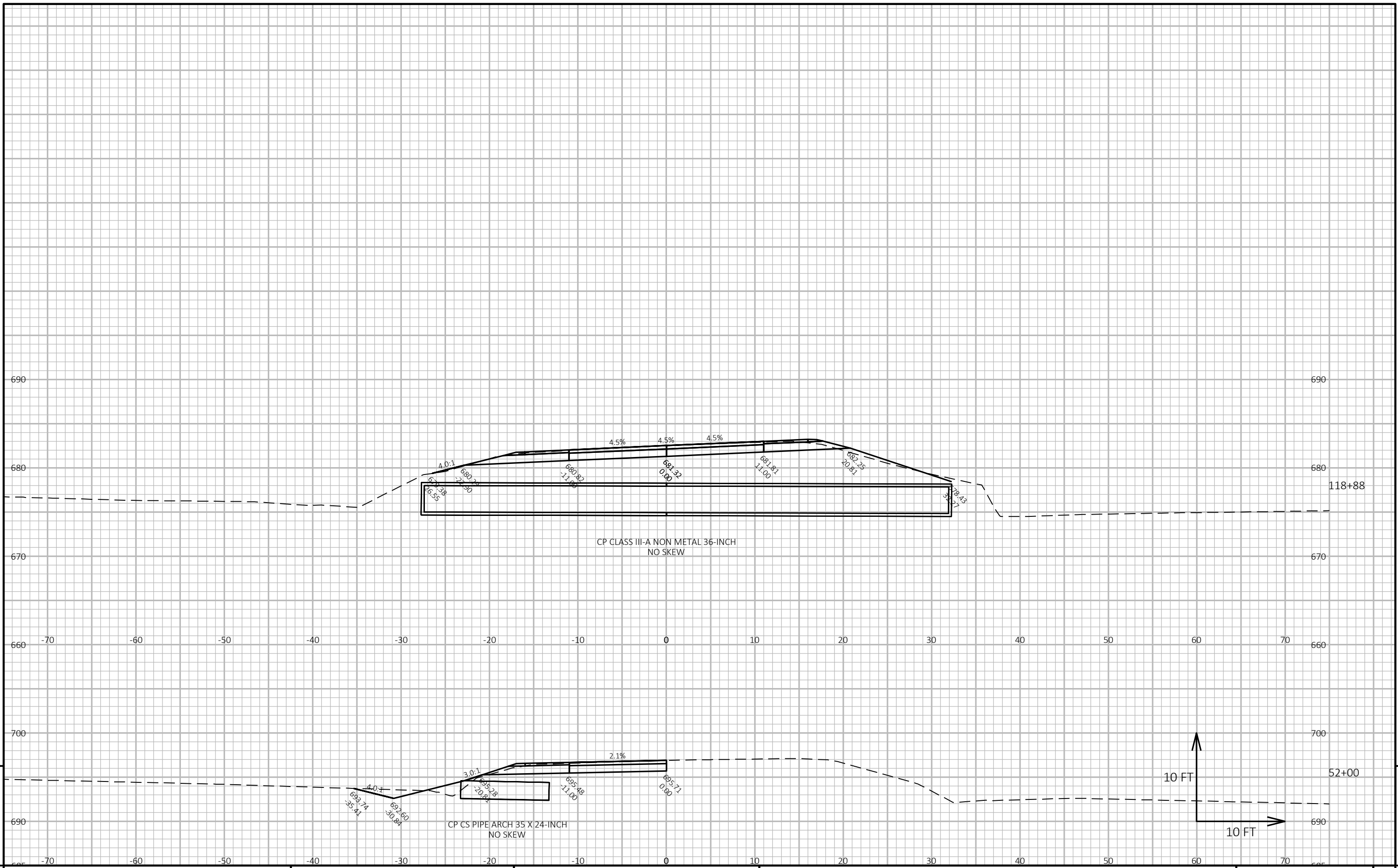
PROJECT NO: 4075-35-71/72 HWY: STH 96 COUNTY: BROWN & OUTAGAMIE CROSS SECTIONS: DITCH SHEET 9



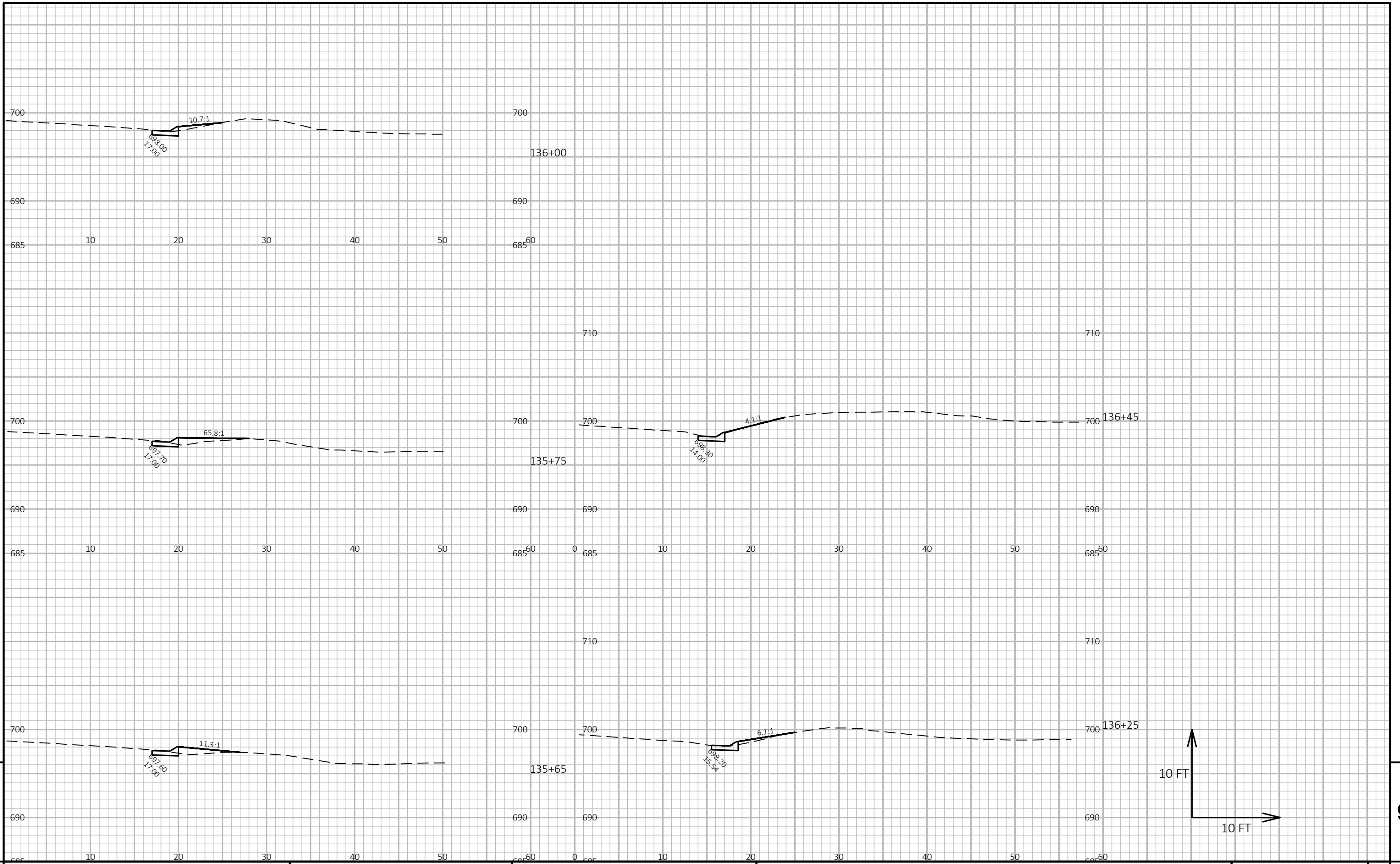
9

9

PROJECT NO: 4075-35-71/72	HWY: STH 96	COUNTY: BROWN & OUTAGAMIE	CROSS SECTIONS: DITCH	SHEET	E
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PROJECT NO: 4075-35-71/72	HWY: STH 96	COUNTY: BROWN & OUTAGAMIE	CROSS SECTIONS: CULVERTS	SHEET	E
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9

9

PROJECT NO: 4075-35-71/72

HWY: STH 96

COUNTY: BROWN & OUTAGAMIE

CROSS SECTIONS: CURB

SHEET

E

FILE NAME : I:\45\450532 STH 96 OUTAGAMIE CO\C3D\SHEETSPLAN\090201-XS.DWG
LAYOUT NAME - 090208-xs

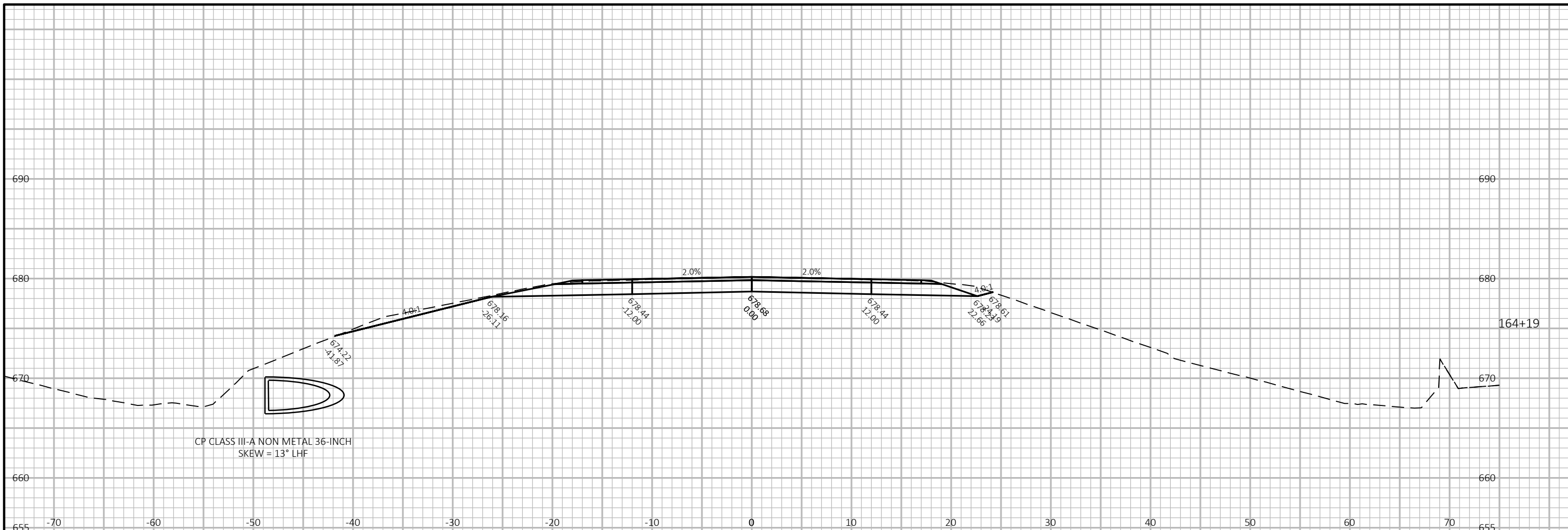
PLOT DATE : 7/21/2023 1:03 PM

PLOT BY : ROWELL, ANDREW

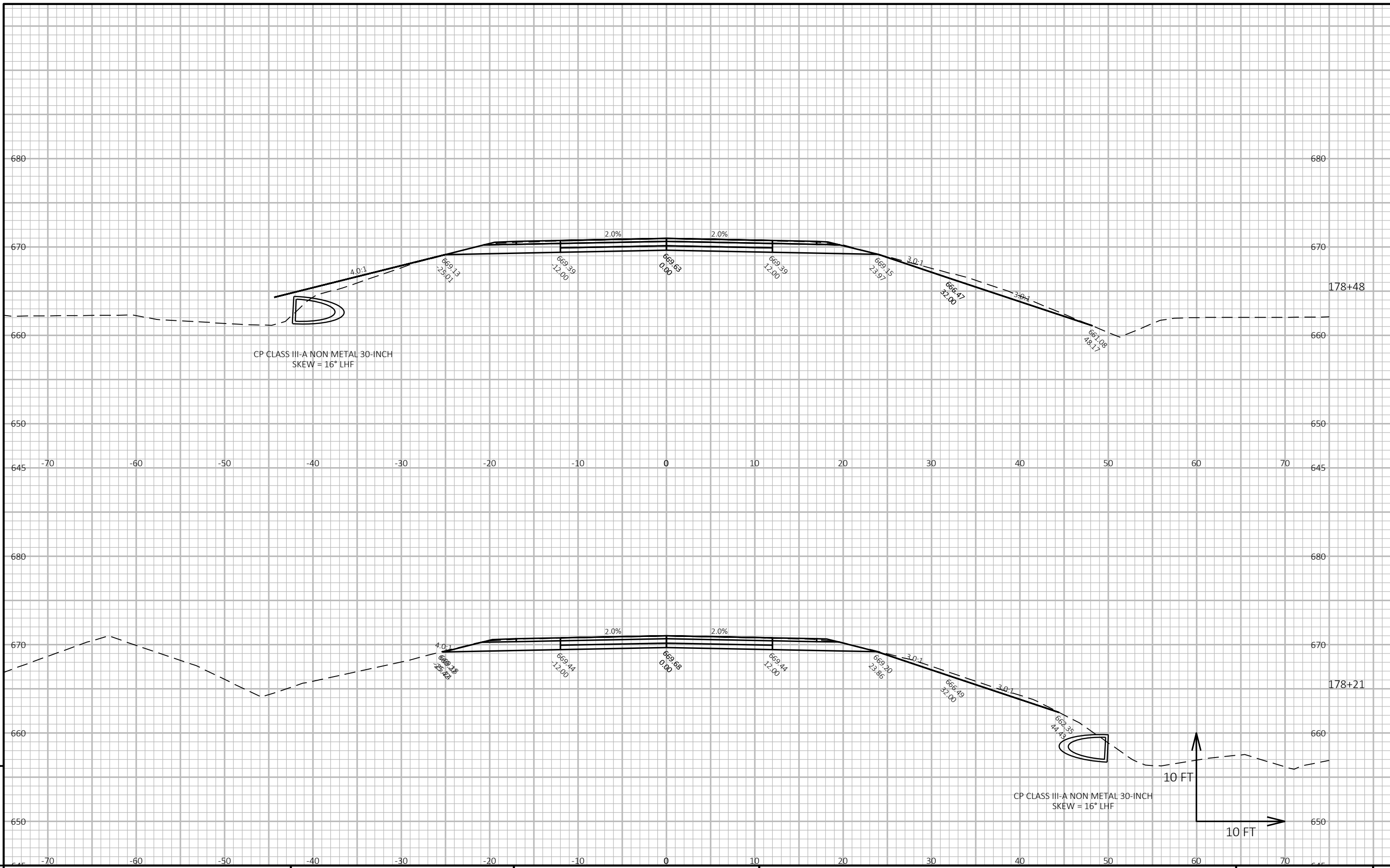
PLOT NAME :

PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.

WISDOT/CADD SHEET 49



PROJECT NO: 4075-35-71/72 HWY: STH 96 COUNTY: BROWN & OUTAGAMIE CROSS SECTIONS: CULVERTS SHEET E

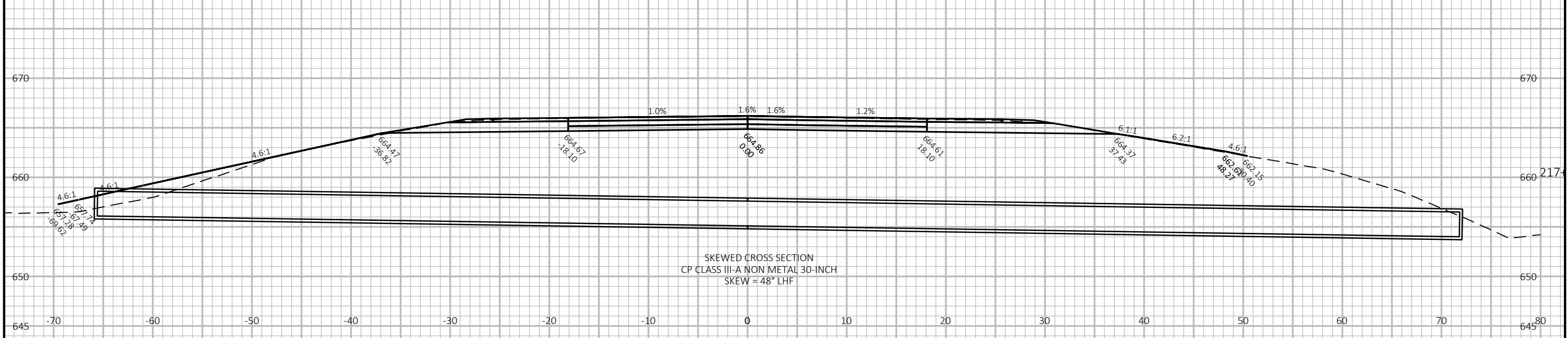


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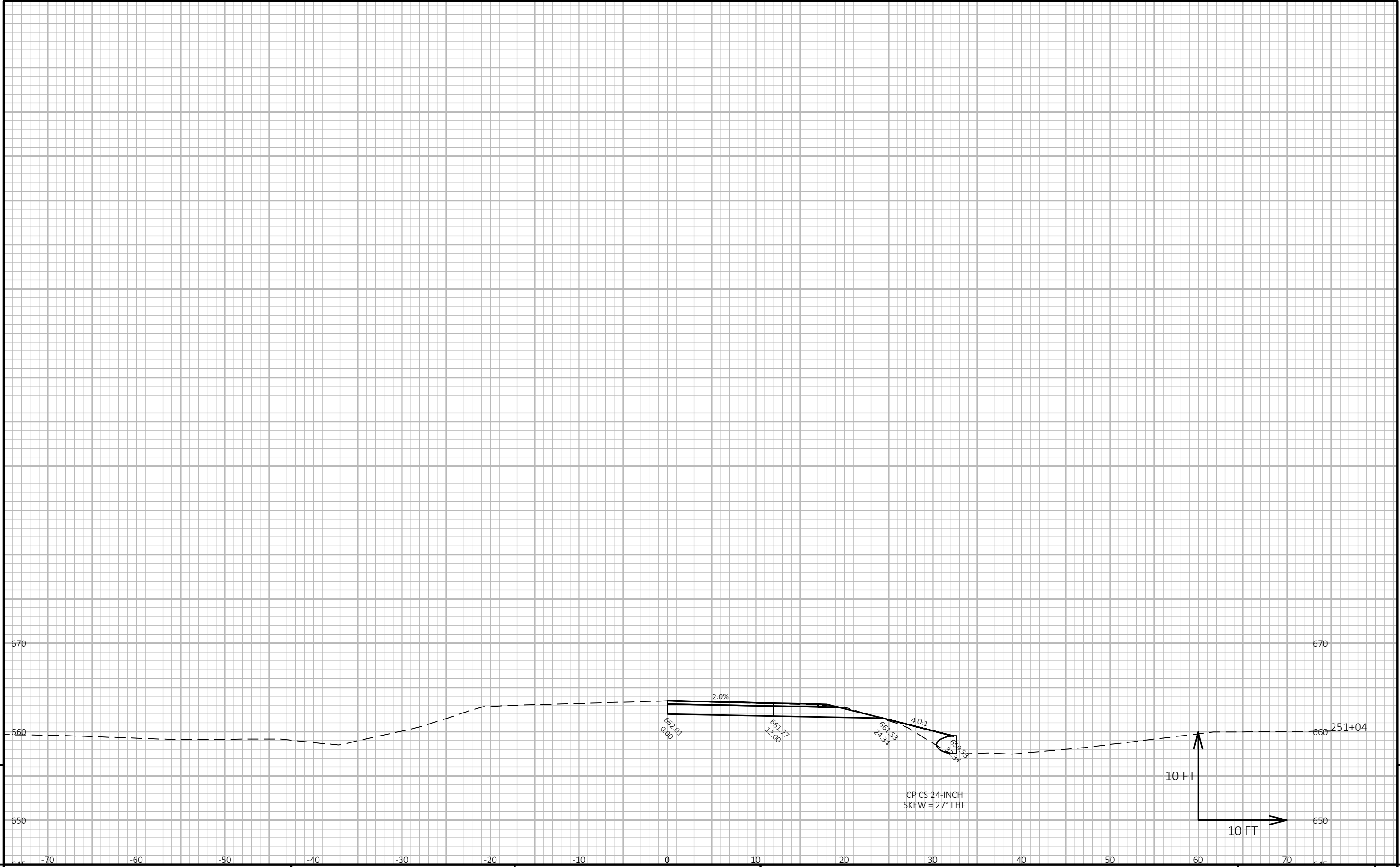
9

PROJECT NO: 4075-35-71/72 HWY: STH 96 COUNTY: BROWN & OUTAGAMIE CROSS SECTIONS: CULVERTS SHEET E

FILE NAME: I:\45\450532 STH 96 OUTAGAMIE CO\C3D\SHEETSPLAN\090201-XS.DWG PLOT DATE: 7/21/2023 1:04 PM PLOT BY: ROWELL, ANDREW PLOT NAME: PLOT SCALE: 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49



9	PROJECT NO: 4075-35-71/72	HWY: STH 96	COUNTY: BROWN & OUTAGAMIE	CROSS SECTIONS: CULVERTS	SHEET	9
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PROJECT NO: 4075-35-71/72	HWY: STH 96	COUNTY: BROWN & OUTAGAMIE	CROSS SECTIONS: CULVERTS	SHEET	E
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FILE NAME : I:\45\450532 STH 96 OUTAGAMIE CO\C3D\SHEETSPLAN\090201-XS.DWG
 LAYOUT NAME - 090212-xs
 PLOT DATE : 7/21/2023 1:04 PM
 PLOT BY : ROWELL, ANDREW
 PLOT NAME :
 PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT.
 WISDOT/CADD SHEET 49



Wisconsin Department of Transportation

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