

ORDER OF SHEETS

Section No.	1	Title
Section No.	2	Typical Sections and Details
Section No.	3	Estimate of Quantities
Section No.	3	Miscellaneous Quantities
Section No.	4	Right of Way Plat
Section No.	5	Plan and Profile
Section No.	6	Standard Detail Drawings
Section No.	7	Sign Plates
Section No.	8	Structure Plans
Section No.	9	Computer Earthwork Data
Section No.	9	Cross Sections

TOTAL SHEETS = 190

PROJECT ID:
5685-00-05

WITH: 5685-00-06

COUNTY:

DANE COUNTY



08

DESIGN DESIGNATION

A.A.D.T. (2020)	=	1793
A.A.D.T. (2040)	=	2264
D.H.V.	=	200
D.D.	=	60/40
T.	=	10%
DESIGN SPEED	=	25 MPH
ESALS	=	130,000

CONVENTIONAL SYMBOLS

PLAN	
CORPORATE LIMITS	
PROPERTY LINE	
LOT LINE	
LIMITED HIGHWAY EASEMENT	
EXISTING RIGHT OF WAY	
PROPOSED OR NEW R/W LINE	
SLOPE INTERCEPT	
REFERENCE LINE	
EXISTING CULVERT	
PROPOSED CULVERT (Box or Pipe)	
COMBUSTIBLE FLUIDS	
MARSH AREA	
WOODED OR SHRUB AREA	

PROFILE	
GRADE LINE	
ORIGINAL GROUND	
MARSH OR ROCK PROFILE (To be noted as such)	
SPECIAL DITCH	
GRADE ELEVATION	
CULVERT (Profile View)	
UTILITIES	
ELECTRIC	
FIBER OPTIC	
GAS	
SANITARY SEWER	
STORM SEWER	
TELEPHONE	
WATER	
UTILITY PEDESTAL	
POWER POLE	
TELEPHONE POLE	

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

VILLAGE OF MCFARLAND, EXCHANGE STREET

FARWELL STREET TO SLEEPY HOLLOW ROAD

LOC STR
DANE COUNTY

STATE PROJECT NUMBER
5685-00-05
(ROADWAY)

STATE PROJECT NUMBER
5685-00-06
(SANITARY SEWER AND WATERMAIN)



BEGIN PROJECT
STA. 12+51
Y = 457,177.68
X = 842,918.89

END PROJECT
STA. 55+80

LAYOUT
SCALE 0 0.5 MI
TOTAL NET LENGTH OF CENTERLINE = 0.823 MI.

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS), DANE COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES. ELEVATIONS ARE REFERENCED TO NAVD 88 (2012). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 12A.

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
5685-00-05	WISC 2024059	1
5685-00-06		

ACCEPTED FOR
VILLAGE OF MCFARLAND
August 1, 2023
(Date)
(Signature and Title of Official)
Matthew G. Schuenke, Village Administrator

ORIGINAL PLANS PREPARED BY

TOWN & COUNTRY
ENGINEERING, INC.

TIMOTHY
STIEVE
44450-6
MADISON,
WI
8/1/2023
(Date)
(Professional Engineer Signature)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
PREPARED BY
Surveyor TOWN & COUNTRY ENGINEERING, INC.
Designer TOWN & COUNTRY ENGINEERING, INC.
Project Manager LORRAINE BETZEL, P.E.
Regional Examiner _____
Regional Supervisor KYLE HEMP, P.E.

APPROVED FOR THE DEPARTMENT
DATE: 8/1/2023
(Signature)

GENERAL NOTES

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLAN ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

THE CONTRACTOR SHALL COORDINATE THEIR CONSTRUCTION ACTIVITIES WITH A CALL TO DIGGERS HOTLINE AND/OR A DIRECT CALL TO UTILITIES THAT HAVE FACILITIES IN THE AREA. NOT ALL UTILITIES ARE MEMBERS OF DIGGERS HOTLINE.

NO TREES OR SHRUBS ARE TO BE REMOVED UNLESS SUCH TREES OR SHRUBS HAVE FIRST BEEN INDICATED FOR REMOVAL BY THE ENGINEER IN THE FIELD.

EXCAVATION BELOW SUBGRADE (EBS) IS NOT USED TO BALANCE YARDAGE, AND IS NOT SHOWN ON THE CROSS SECTIONS BUT IS MEASURED AND PAID FOR AS COMMON EXCAVATION. EXACT LOCATIONS OF EBS WILL BE DETERMINED BY THE ENGINEER.

DISTURBED AREAS SHOWN WITHIN THE PROJECT LIMITS ARE TO BE FERTILIZED, SEEDED, AND MULCHED AS DIRECTED BY THE ENGINEER.

WHEN THE QUANTITY OF THE ITEM OF BASE AGGREGATE DENSE OR HMA PAVEMENT IS MEASURED FOR PAYMENT BY THE TON, THE DEPTH OR THICKNESS OF THE COURSE SHOWN ON THE PLANS IS APPROXIMATE, AND THE ACTUAL THICKNESS WILL DEPEND ON THE DISTRIBUTION OF THE MATERIAL AS DIRECTED BY THE ENGINEER IN THE FIELD.

SILT FENCE, AND INLET PROTECTION BID ITEMS SHALL BE PLACED AS SHOWN IN THE PLAN OR AS DIRECTED BY THE ENGINEER. EROSION CONTROL SHALL BE PLACED PRIOR TO CONSTRUCTION.

FILL EXPANSION IS VARIABLE AND IS ESTIMATED AT 25%.

THE EXACT LOCATION OF PRIVATE AND FIELD ENTRANCES TO BE DETERMINED BY THE ENGINEER IN THE FIELD.

REMOVAL OF ASPHALTIC SURFACES WERE AN ABUTTING ASPHALTIC SURFACE IS TO REMAIN IN PLACE SHALL REQUIRE A SAWCUT MEETING THE APPROVAL OF THE ENGINEER IN THE FIELD.

ACCURACY OF INLET AND DISCHARGE ELEVATIONS FOR DRAINAGE STRUCTURES SHALL BE VERIFIED BY THE ENGINEER IN THE FIELD.

CURB AND GUTTER ELEVATIONS ARE GIVEN ON THE FLANGE LINE, UNLESS OTHERWISE NOTED.

ALL RADII DIMENSIONS ON THE PLAN FOR CURB AND GUTTER ARE TO THE FLANGE OF THE CURB AND GUTTER.

EXISTING DRIVEWAYS SHALL BE RESTORED IN KIND (UNLESS OTHERWISE NOTED) AND THEIR LOCATION VERIFIED BY THE ENGINEER IN THE FIELD.

HMA PAVMENT QUANTITIES WERE CALCULATED USING 115 LB/SY/IN. 4.5-INCHES OF HMA PAVEMENT SHALL BE CONSTRUCTED WITH A 1.5-INCH UPPER LAYER HMA PAVEMENT 5LT58-28S AND A 3-INCH LOWER LAYER HMA PAVEMENT 4LT 58-28S.

EXPANSION JOINTS SHALL BE CONSTRUCTED AT ALL RADII POINTS IN THE CURB & GUTTER.

STORM SEWER ELEVATIONS, LENGTHS AND LOCATIONS AS SHOWN ON THE PLANS AND CROSS SECTIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.

MISCELLANEOUS REMOVAL ITEMS REQUIRING RESTORATIONS OF CONCRETE OR ASPHALT DRIVEWAYS, CONCRETE DRIVEWAYS, OR SIDE ROADS/HIGHWAYS SHALL BE REMOVED TO AN EXISTING JOINT OR SAWED AS DETERMINED BY THE ENGINEER IN THE FIELD OR AS SHOWN ON THE PLANS.

CURVE DATA IS BASED ON THE ARC DEFINITION.

THE LOCATION OF ALL PERMANENT SIGNING SHALL BE VERIFIED BY THE ENGINEER IN THE FIELD PRIOR TO PLACEMENT.

ADJUST DITCH GRADING AS NECESSARY TO FIELD CONDITIONS AND AS DIRECTED BY THE ENGINEER IN THE FIELD.

WETLANDS ARE PRESENT WITHIN THE PROJECT LIMITS. THE CONTRACTOR SHALL NOT OPERATE OR STOCKPILE EQUIPMENT BEYOND THE EXISTING TOE OF SLOPE AT STA. 95+50 - STA. 97+50, RT.

THE SOILS REPORT MAY BE OBTAINED FROM THE ENGINEER.



CONTACTS

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WDNR LIAISON

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WATER DISTRIBUTION

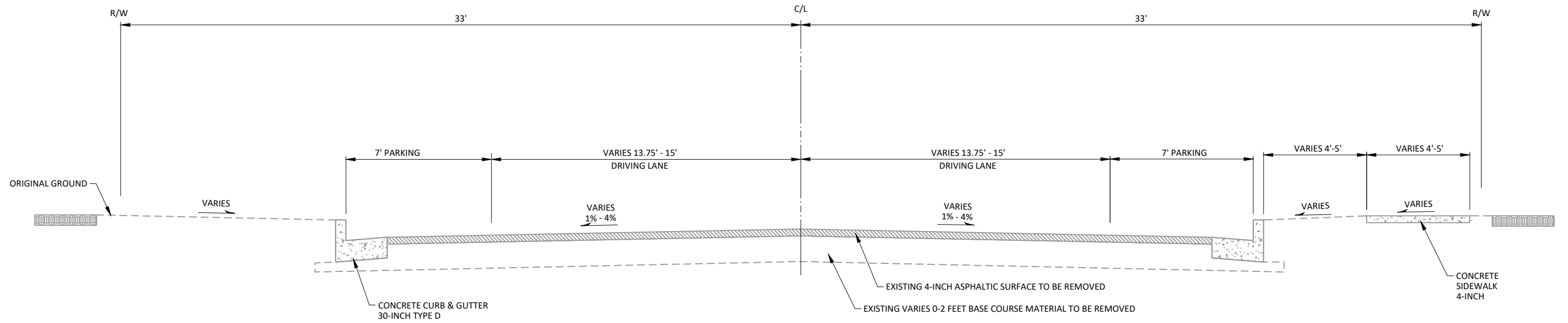
VILLAGE OF MCFARLAND
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LIST OF STANDARD ABBREVIATIONS

ABUT	Abutment	INV	Invert	RDWY	Roadway
AC	Acre	IP	Iron Pipe or Pin	SALV	Salvaged
AGG	Aggregate	IRS	Iron Rod Set	SAN S	Sanitary Sewer
AH	Ahead	JT	Joint	SEC	Section
<	Angle	JCT	Junction	SHLDR	Shoulder
ASPH	Asphaltic	LHF	Left-Hand Forward	SHR	Shrinkage
AVG	Average	L	Length of Curve	SW	Sidewalk
ADT	Average Daily Traffic	LIN FT	Linear Foot	S	South
BAD	Base Aggregate Dense	or LF		SQ	Square
BK	Back	LC	Long Chord of Curve	SF or SQ FT	Square Feet
BF	Back Face	MH	Manhole	SY or SQ YD	Square Yard
BM	Bench Mark	MB	Mailbox	STD	Standard
BR	Bridge	ML or M/L	Match Line	SDD	Standard Detail Drawings
C or C/L	Center Line	N	North	STH	State Trunk Highways
CC	Center to Center	Y	North Grid Coordinate	STA	Station
C.E.	Commercial Entrance	OD	Outside Diameter	SS	Storm Sewer
CTH	County Trunk Highway	PLE	Permanent Limited Easement	SG	Subgrade
CR	Creek	PT	Point	SE	Superelevation
CR	Crushed	PC	Point of Curvature	SL or S/L	Survey Line
CY or CU YD	Cubic Yard	PI	Point of Intersection	SV	Septic Vent
CP	Culvert Pipe	PRC	Point of Reverse Curvature	T	Tangent
C & G	Curb and Gutter			TEL	Telephone
D	Degree of Curve	PT	Point of Tangency	TEMP	Temporary
DHV	Design Hour Volume	POC	Point On Curve	TI	Temporary Interest
DIA	Diameter	POT	Point on Tangent	TLE	Temporary Limited Easement
E	East	PVC	Polyvinyl Chloride		
X	East Grid Coordinate	PCC	Portland Cement Concrete	t	Ton
ELEC	Electric (al)	LB	Pound	T or TN	Town
EL or ELEV	Elevation	PSI	Pounds Per Square Inch	TRANS	Transition
ESALS	Equivalent Single Axle Loads	P.E.	Private Entrance	TL or T/L	Transit Line
EBS	Excavation Below Subgrade	R	Radius	T	Trucks (percent of)
FF	Face to Face	RR	Railroad	TYP	Typical
F.E.	Field Entrance	R	Range	UNCL	Unclassified
F	Fill	RL or R/L	Reference Line	UG	Underground Cable
FG	Finished Grade	RP	Reference Point	USH	United States Highway
FL or F/L	Flow Line	RCCP	Reinforced Concrete Culvert Pipe	VAR	Variable
FT	Foot	REQD	Required	V	Velocity or Design Speed
FTG	Footing	RES	Residence or Residential	VERT	Vertical
GN	Grid North	RW	Retaining Wall	VC	Vertical Curve
HT	Height	RT	Right	VOL	Volume
CWT	Hundredweight	RHF	Right-Hand Forward	WM	Water Main
HYD	Hydrant	R/W	Right-of-Way	WV	Water Valve
INL	Inlet	RD	Road	W	West
ID	Inside Diameter	RD	Road	WB	Westbound
		R	River	YD	Yard

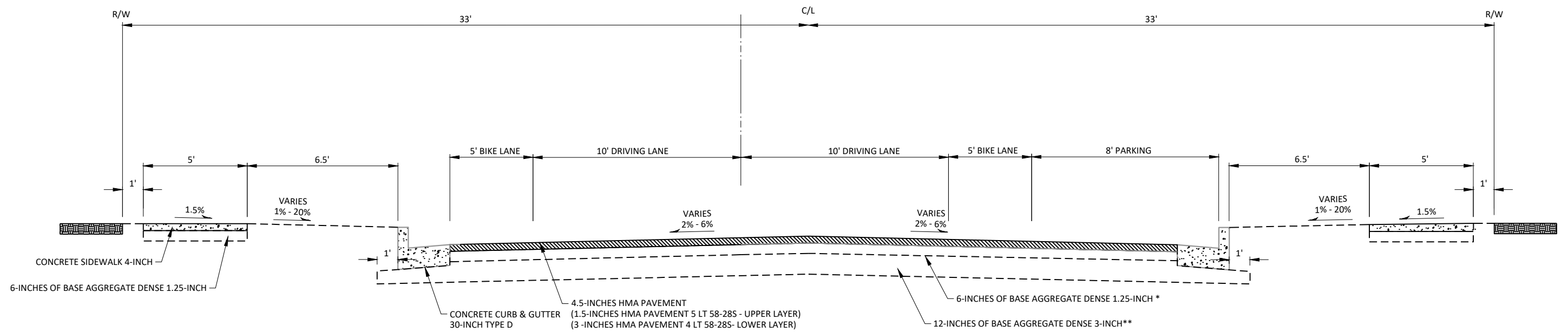
ORDER OF SECTION 2 SHEETS:

- WRITTEN MATERIAL
- PROJECT OVERVIEW
- TYPICAL SECTIONS
- CONSTRUCTION DETAILS
- INTERSECTION AND ALIGNMENT DETAILS
- TRAFFIC CONTROL
- CONTROL POINT TIES
- CURB RAMP DETAILS
- STORM SEWER PLAN
- SANITARY SEWER AND WATERMAIN DETAILS
- LIGHTING PLAN
- PERMANENT SIGNING/PAVEMENT MARKING



TYPICAL EXISTING SECTION

EXCHANGE STREET
STA. 12+51 - STA. 55+80

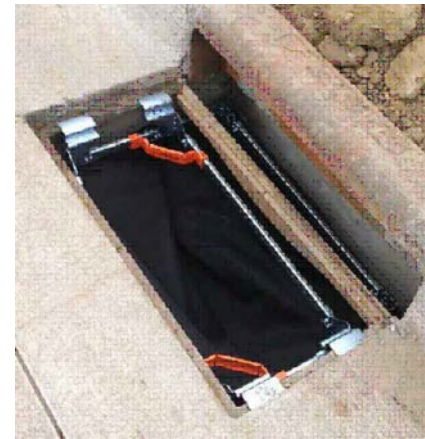
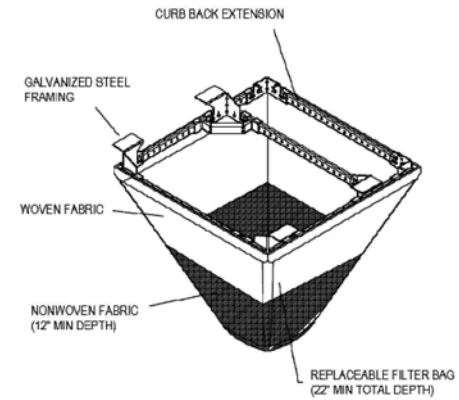


TYPICAL FINISHED SECTION

EXCHANGE STREET
STA. 12+51 - STA. 55+80

*THE CROWN OF THE ROAD SHALL BE CREATED USING THE BASE AGGREGATE DENSE 1.25-INCH. THE THICKNESS SHOWN IS THE MINIMUM THICKNESS REQUIRED AS MEASURED AT THE CONCRETE CURB AND GUTTER SECTION.

**AN UNDISTRIBUTED QUANTITY OF SELECT CRUSHED MATERIAL IS PROVIDED FOR USE AS EBS FILL.



GENERAL NOTES:

WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED IN THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

FRAMED INLET PROTECTION SHALL BE COMPLIANT WITH ALL ASTM STANDARD D8057-17 REQUIREMENTS, INCLUDING:
A. BYPASS OVERFLOW THAT MEETS OR EXCEEDS INLET DESIGN FLOW
B. FRAME AND BAG STRONG ENOUGH TO HANDLE FULL SEDIMENT LOAD.

INSTALLATION NOTES:

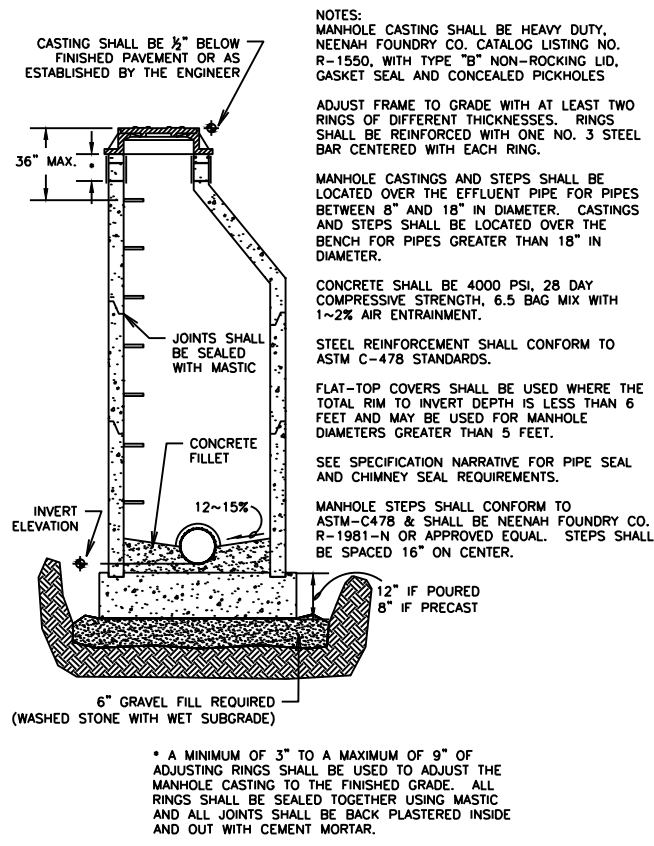
NO PART OF INLET PROTECTION SHALL BE PROJECTING ABOVE THE GRATE.

FOR COMBINATION INLETS, PROTECTION SHALL CAPTURE RUNOFF ENTERING BOTH GRATE AND CURB OPENING.

A DUAL FABRIC FILTER BAG, WITH NON-WOVEN BOTTOM AND WOVEN TOP SHALL BE USED.

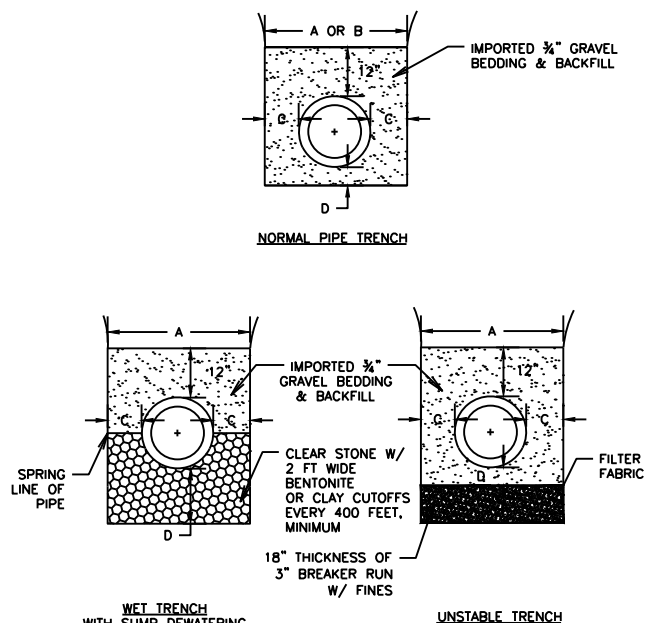
THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.

DETAIL
INLET PROTECTION D-RF - FRAMED (W/ CURB BOX)

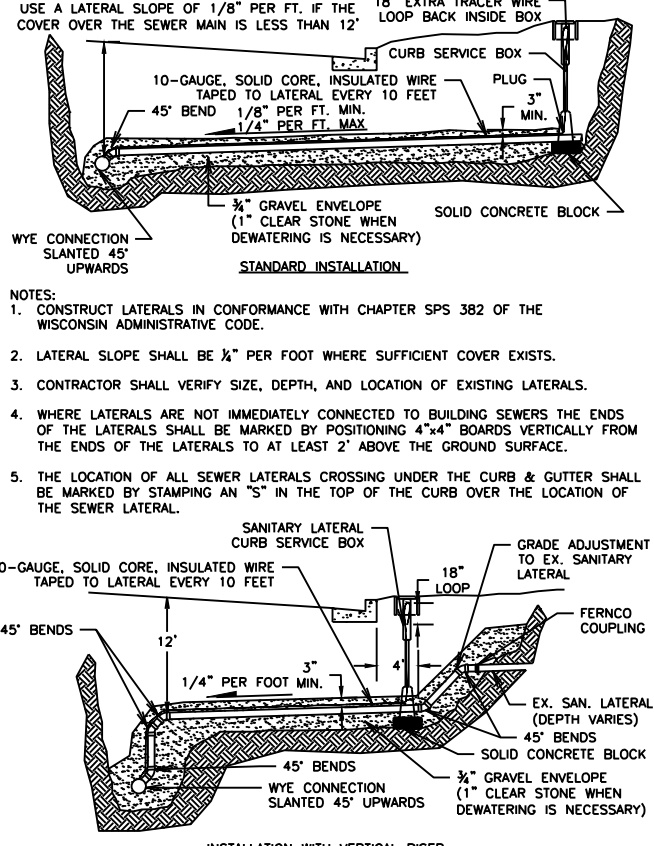


DETAIL
MANHOLE

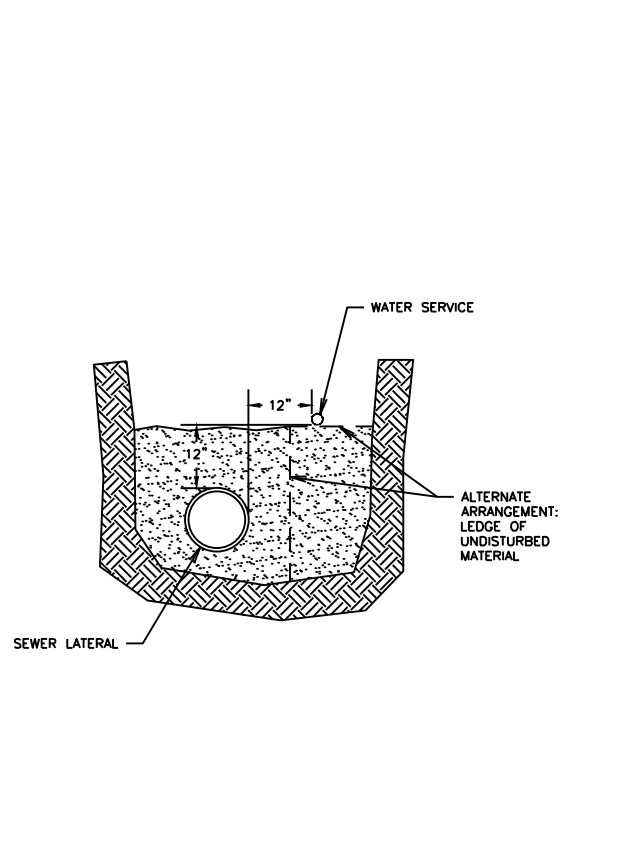
DIMENSIONS:
 A: OUTSIDE DIAMETER OF PIPE PLUS 24" MAXIMUM, EXCEPT NEED NOT BE LESS THAN 36". TRENCH SHIELDS NARROWER THAN 4 FEET INSIDE WIDTH WILL NOT BE REQUIRED UNLESS SPECIFICALLY REQUIRED IN THE PROJECT SPECIFICATIONS.
 B: FOR ROCK, OUTSIDE DIAMETER OF PIPE PLUS 18" MAXIMUM, EXCEPT NEED NOT BE LESS THAN 36".
 C: MINIMUM - 6"
 D: MINIMUM 4" BELOW BARREL AND 3" BELOW BELL.



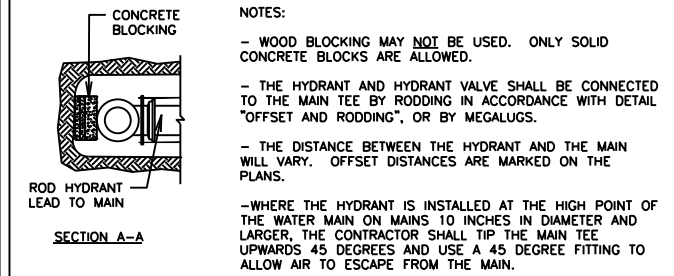
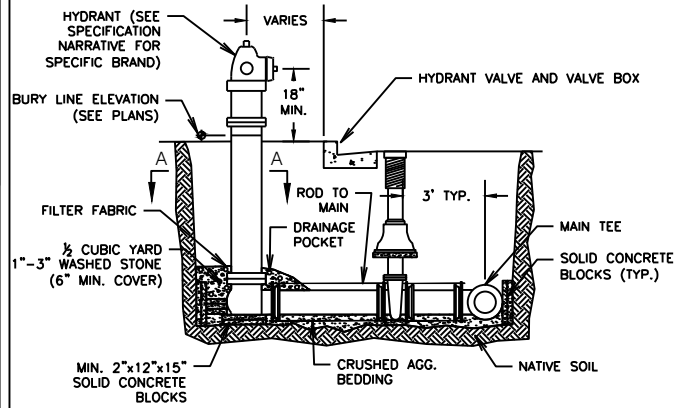
DETAIL
TRENCH WIDTH AND BEDDING



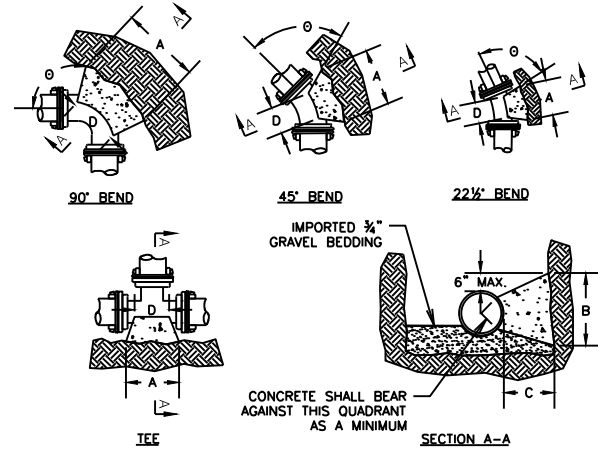
DETAIL
SANITARY SEWER LATERAL



DETAIL
JOINT TRENCH INSTALLATION



DETAIL HYDRANT SETTING



WOOD BLOCKING MAY NOT BE USED. ONLY SOLID CONCRETE BLOCKS ARE ALLOWED.

DIMENSION "D" SHALL BE AS LARGE AS POSSIBLE, BUT THE CONCRETE SHALL NOT INTERFERE WITH THE MECHANICAL JOINTS.

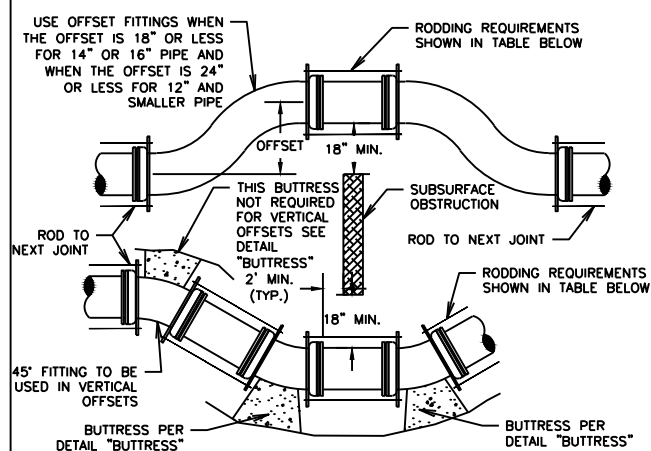
DIMENSION "C" SHALL BE AT LEAST 6 INCHES, AND LARGE ENOUGH TO MAKE THE "θ" ANGLE EQUAL TO OR GREATER THAN 45 DEGREES WITH THE DIMENSION "A" AS SHOWN ON THE TABLE, OR GREATER, AND WITH DIMENSION "D" AS LARGE AS POSSIBLE.

CONCRETE SHALL BE CLASS "CC". SEE SECTION 03301.

PIPE SIZE	TEES			22.5° BEND			45° BEND			90° BEND		
	A	B	D	A	B	D	A	B	D	A	B	D
6	1'-3"	1'-0"	1'-0"	1'-0"	1'-0"	1'-0"	1'-0"	1'-0"	1'-0"	1'-4"	1'-2"	1'-6"
8	1'-6"	1'-4"	1'-0"	1'-0"	1'-0"	1'-4"	1'-2"	1'-10"	1'-10"	1'-10"	1'-6"	1'-6"
10	2'-3"	2'-0"	1'-4"	1'-4"	1'-10"	1'-10"	2'-8"	2'-8"	2'-8"	2'-8"	2'-3"	2'-3"
14	3'-2"	2'-8"	1'-10"	1'-8"	2'-6"	2'-4"	3'-10"	2'-10"	2'-10"	2'-10"	2'-10"	2'-10"
18	4'-0"	3'-0"	2'-4"	2'-0"	3'-3"	2'-10"	5'-0"	3'-4"	3'-4"	3'-4"	3'-4"	3'-4"
22/24	5'-3"	3'-4"	2'-10"	2'-4"	4'-0"	3'-3"	6'-4"	3'-10"	3'-10"	3'-10"	3'-10"	3'-10"
30	6'-3"	4'-3"	3'-6"	3'-0"	5'-4"	3'-10"	8'-0"	4'-8"	4'-8"	4'-8"	4'-8"	4'-8"

DIMENSIONS IN THE TABLE ARE BASED ON A WATER PRESSURE OF 150 PSI AND SOIL RESISTANCE OF 200 LBS/SQ.FT.

DETAIL BUTTRESS

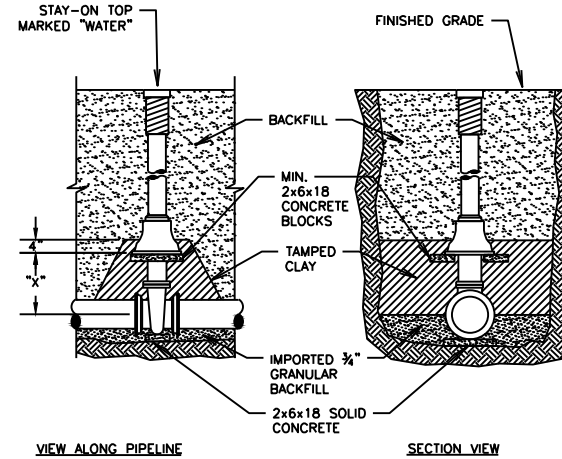


NOMINAL PIPE SIZE	RODS NO.	RODS DIA.	STRAP SIZE	BOLT DIA.	WASHER SIZE
6	3	3/8"	1/2" x 2"	3/8"	1/2" x 3" x 5"
8	4	3/8"	1/2" x 2"	3/8"	1/2" x 3" x 5"
10	4	3/8"	1/2" x 2 1/2"	1"	1/2" x 3" x 5"
12	4	3/8"	1/2" x 2 1/2"	1"	1/2" x 3" x 5"
14	4	3/8"	1/2" x 2 1/2"	1"	1/2" x 3" x 5"

ALL DIMENSIONS IN THIS TABLE ARE IN INCHES.

NOTES:
 - RODS AND WASHERS TO BE ASTM A-575 MERCHANT QUALITY 0.17-0.24 CARBON. NUTS TO BE AMERICAN STANDARD HEAVY, NOT PRESSED.
 - THE RODS, BOLTS, NUTS, BANDS AND WASHERS TO BE FURNISHED AND ASSEMBLED BY THE CONTRACTOR.
 - ALL STEEL MATERIAL TO BE GALVANIZED OR THOROUGHLY COATED WITH ENGINEER APPROVED COATING.
 - OFFSET FITTINGS REQUIRE CONTINUOUS RODDING IN ALL POSITIONS.
 - VERTICAL OFFSETS SHALL NOT CREATE A HIGH POINT IN THE WATER MAIN. VERTICAL OFFSETS REQUIRE THE SAME RODDING AND BUTTRESSING AS SHOWN ABOVE.
 - MEGALUG RESTRAINTS MAY BE USED IN LIEU OF RODDING.

DETAIL OFFSET AND RODDING

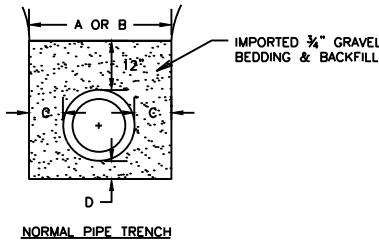


PIPE DIA., INCHES	6	8	10	12	14	16
"x" DIMENSION, INCHES	12	13	17	21	25	30

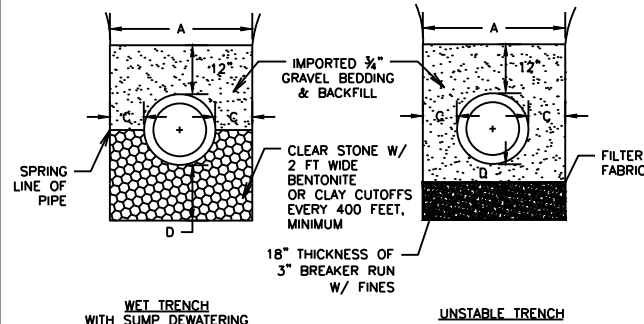
NOTES:
 - SOLID CONCRETE BLOCKS MUST BE USED.
 - VALVES SHALL BE SECURED WITH RODDING OR MEGALUGS TO THE NEAREST "TEE" FITTING OR TO THE FIRST JOINT CONNECTING A FULL SECTION OF WATER MAIN PIPE. SEE RODDING DETAIL "OFFSET AND RODDING".

DETAIL VALVE BOX SETTING

DIMENSIONS:
 A: OUTSIDE DIAMETER OF PIPE PLUS 24" MAXIMUM, EXCEPT NEED NOT BE LESS THAN 36". TRENCH SHIELDS NARROWER THAN 4 FEET INSIDE WIDTH WILL NOT BE REQUIRED UNLESS SPECIFICALLY REQUIRED IN THE PROJECT SPECIFICATIONS.
 B: FOR ROCK, OUTSIDE DIAMETER OF PIPE PLUS 18" MAXIMUM, EXCEPT NEED NOT BE LESS THAN 36".
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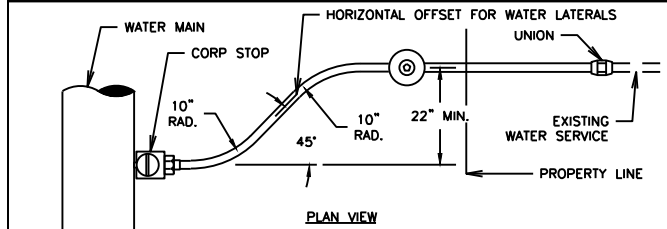
NORMAL PIPE TRENCH



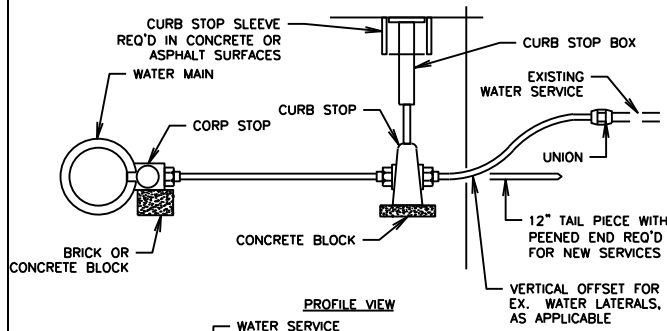
WET TRENCH WITH SUMP DEWATERING

UNSTABLE TRENCH

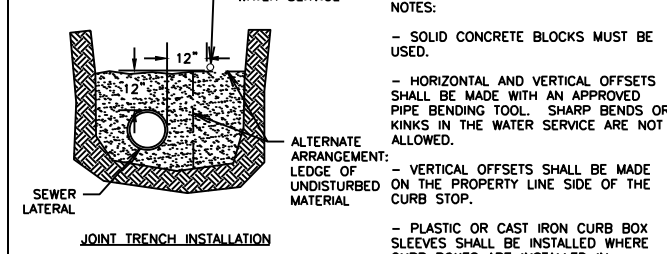
DETAIL TRENCH WIDTH AND BEDDING



PLAN VIEW



PROFILE VIEW





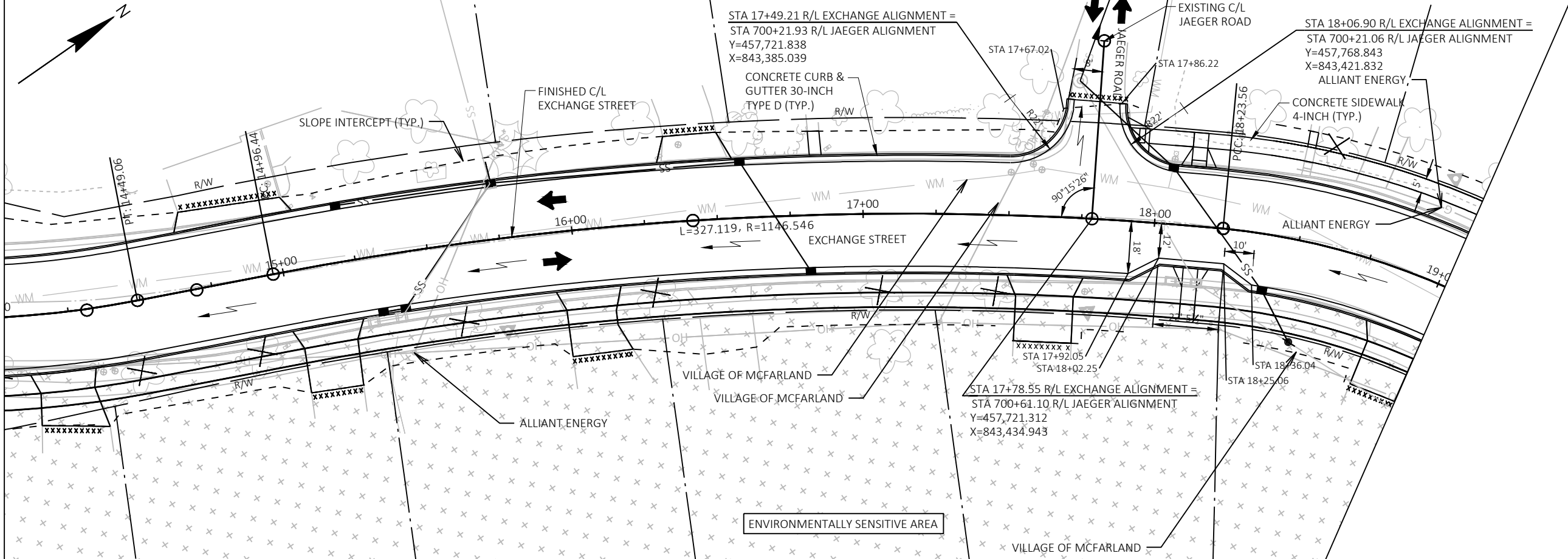
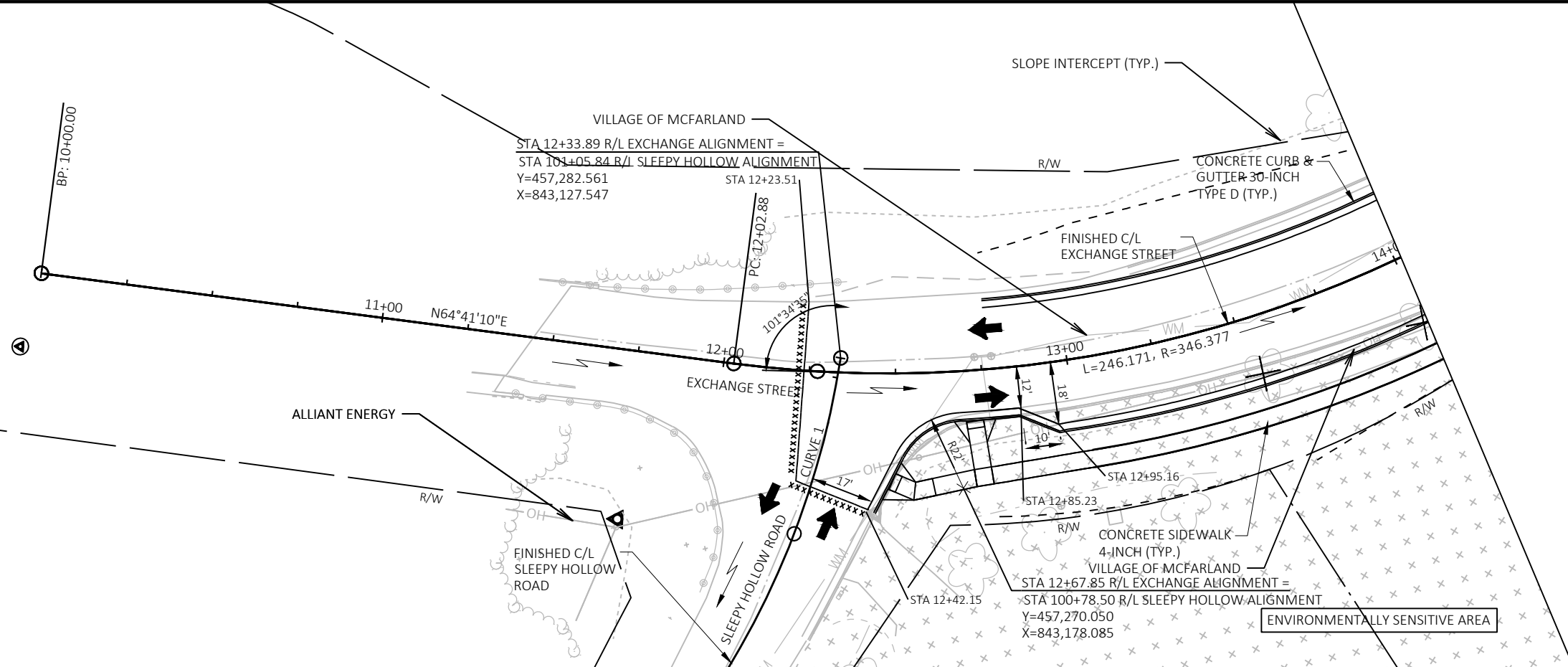
JOINT TRENCH INSTALLATION

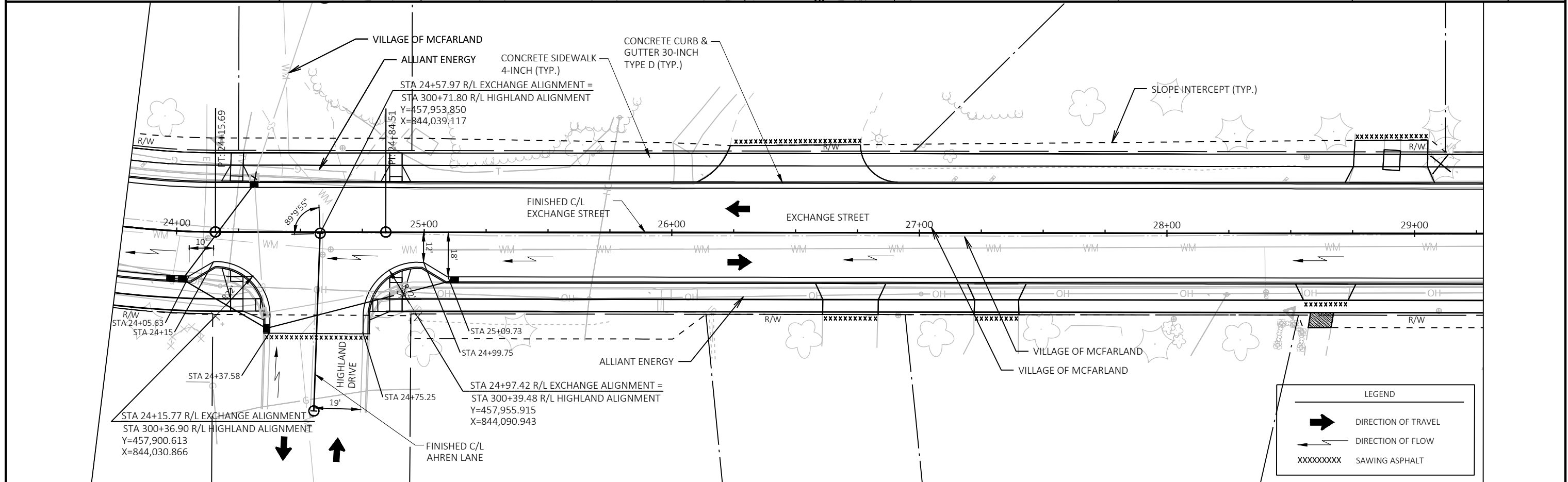
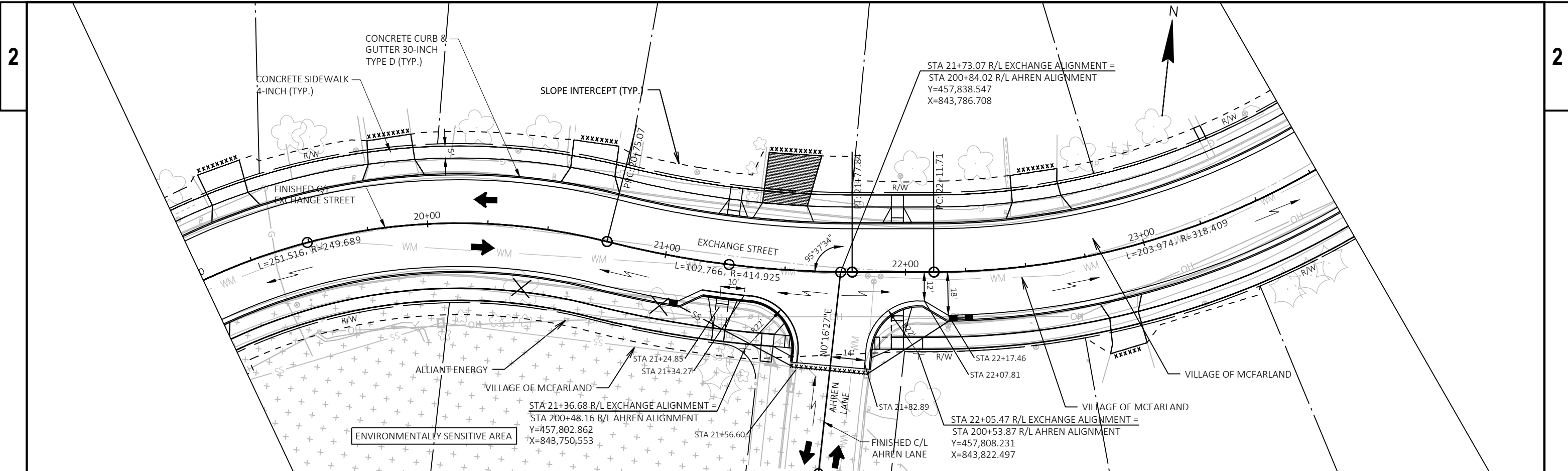
DETAIL WATER SERVICE INSTALLATION

CURVE 1
 PI STA = 100+53.76
 Y = 457233.487
 X = 843149.498
 DELTA = 24°44'15" LT
 D = 23°22'17"
 T = 53.76'
 L = 105.84'
 R = 245.15'
 PC STA = 100+00.00
 Y = 457179.730
 X = 843148.900
 PT STA = 101+05.84
 Y = 457282.561
 X = 843127.547
 DB = N00°38'16"E
 DA = N24°05'59"W

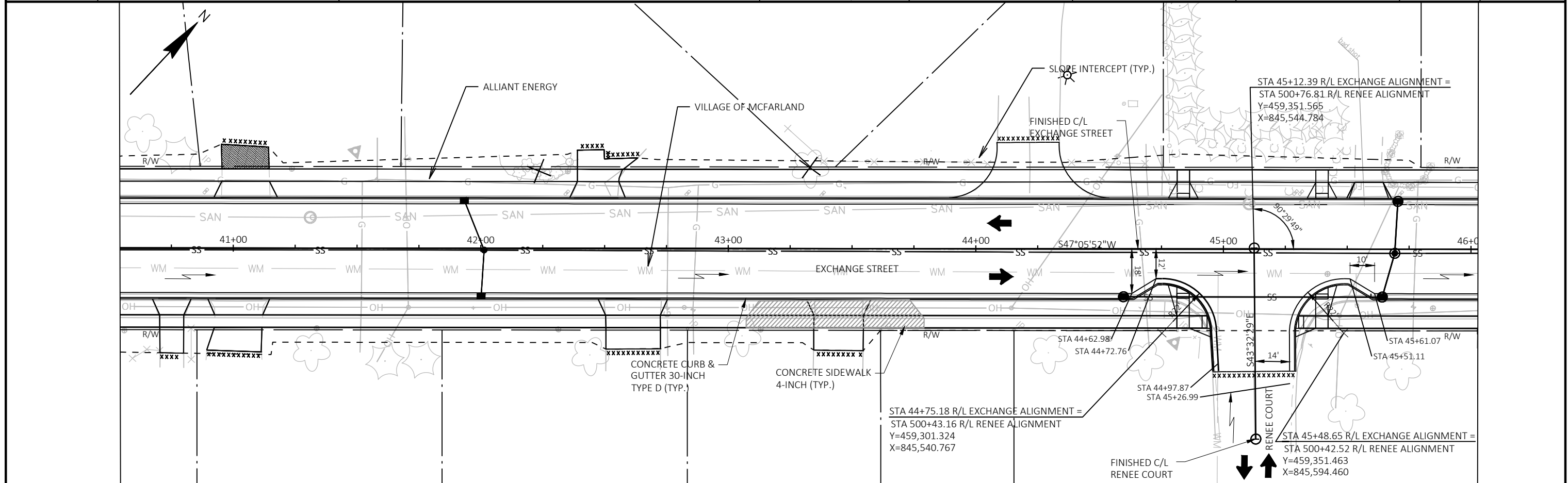
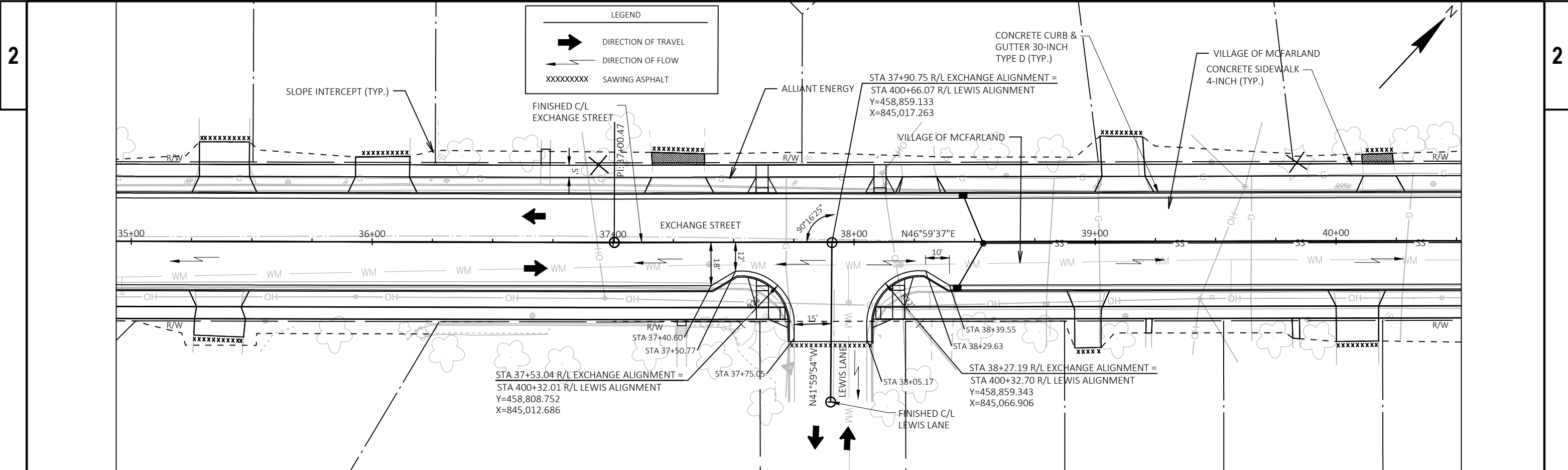
LEGEND

-  DIRECTION OF TRAVEL
-  DIRECTION OF FLOW
- XXXXXXXXXX SAWING ASPHALT

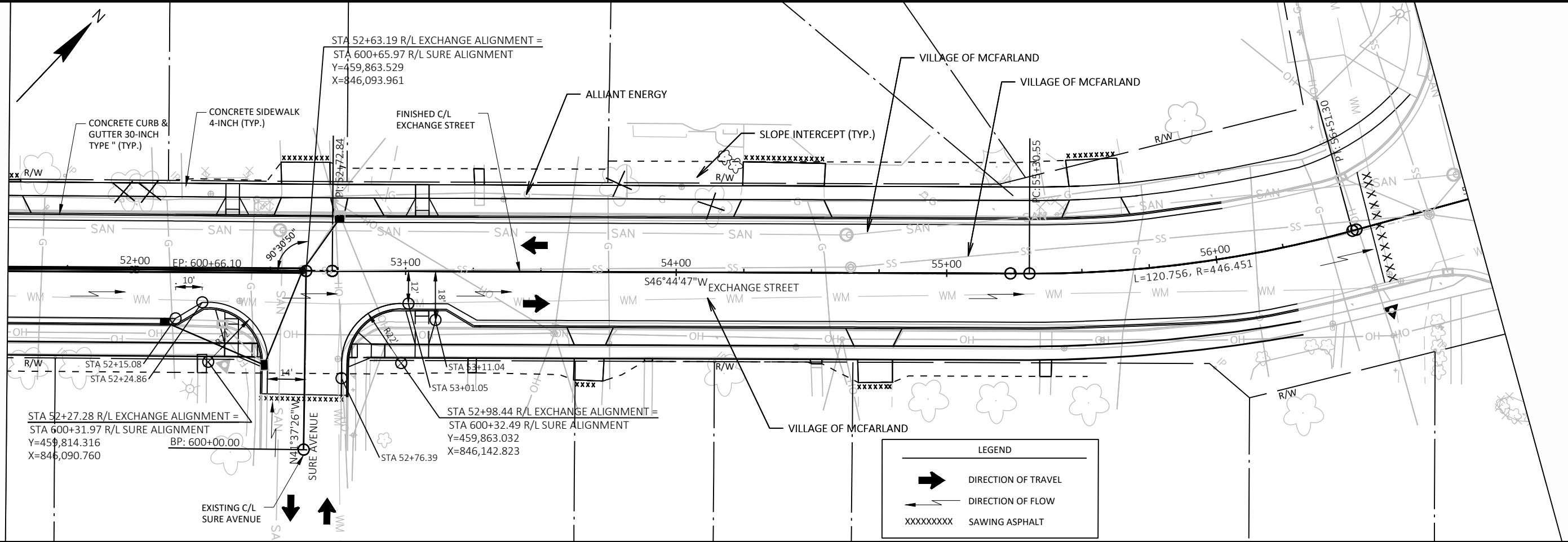




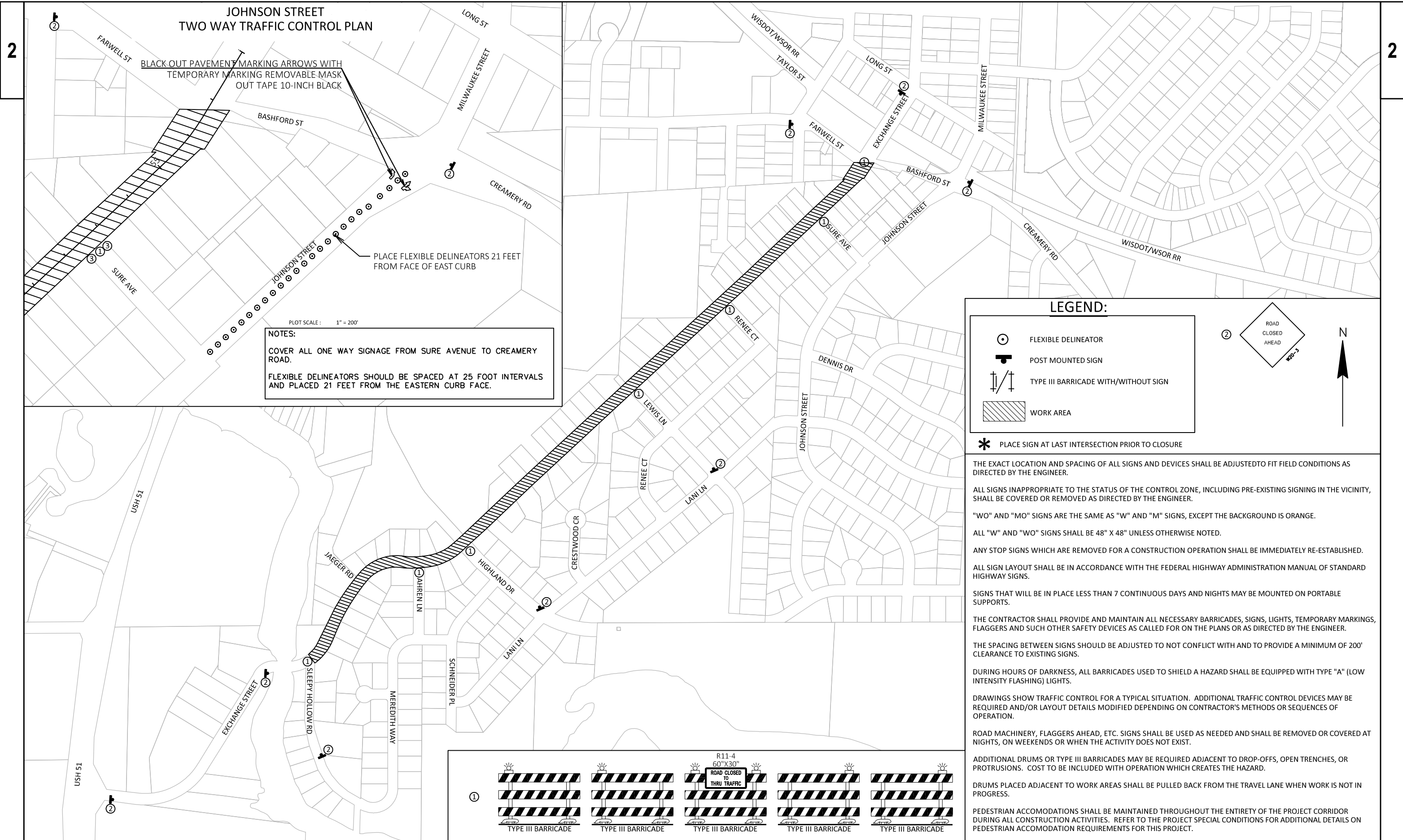
PROJECT NO: 5685-00-05 HWY: EXCHANGE STREET COUNTY: DANE INTERSECTION DETAILS SHEET 9 E

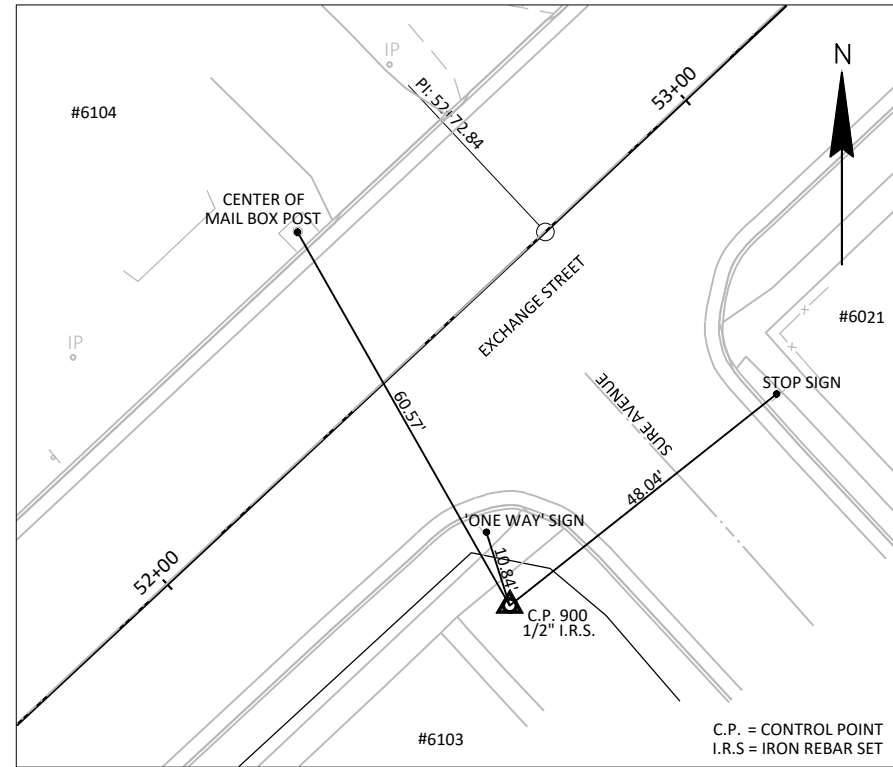


PROJECT NO: 5685-00-05	HWY: EXCHANGE STREET	COUNTY: DANE	INTERSECTION DETAILS	SHEET 10	E
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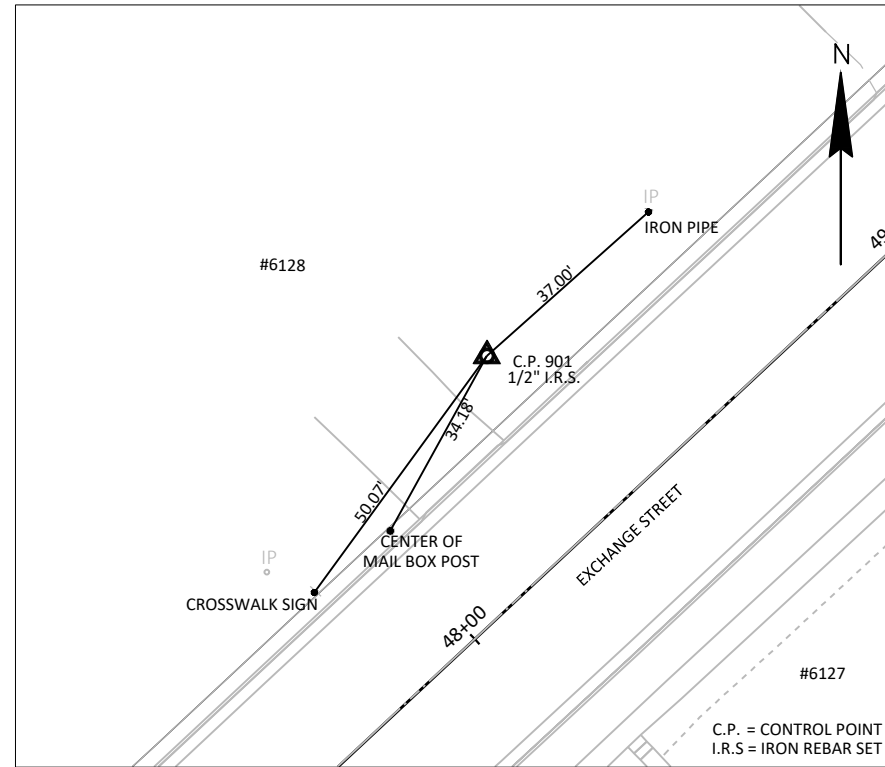


LEGEND	
	DIRECTION OF TRAVEL
	DIRECTION OF FLOW
	SAWING ASPHALT

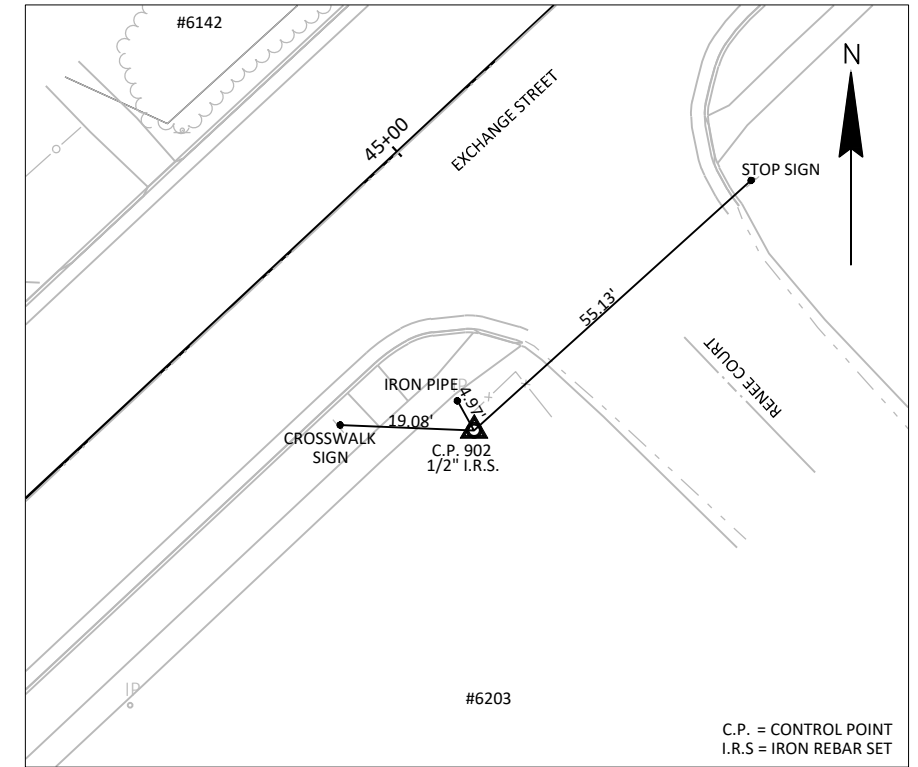




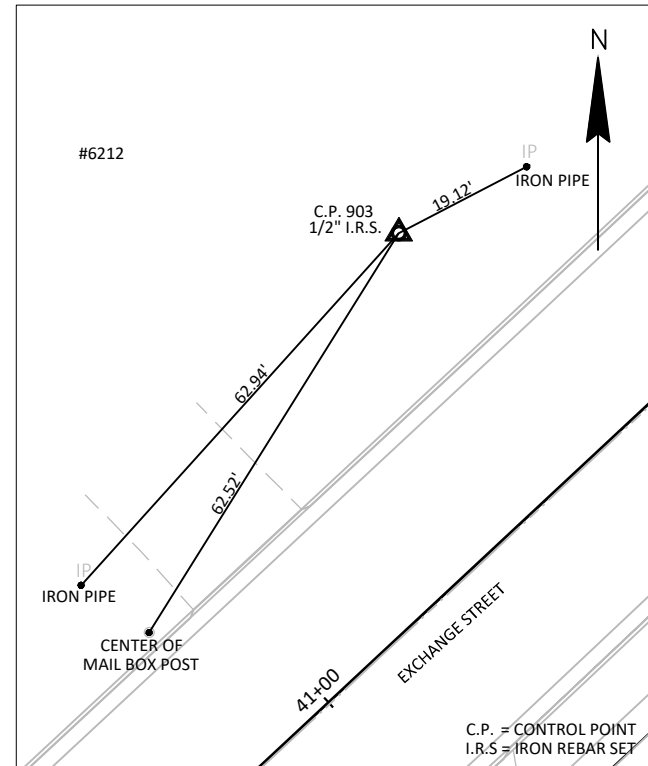
TIES TO C.P.#900
 STA. 52+33.27, 35.08' RT
 Y = 459,817.615
 X = 846,095.874



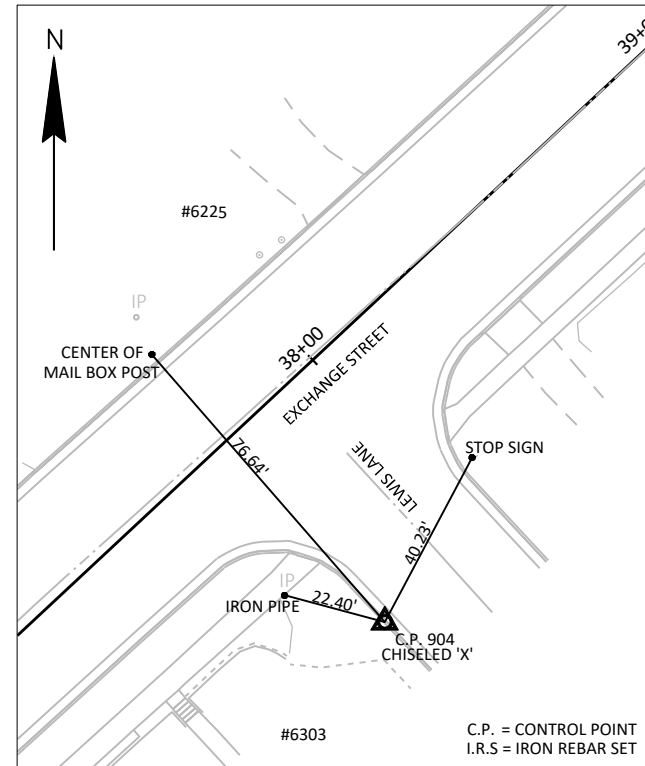
TIES TO C.P.#901
 STA. 48+34.52, 33.96' LT
 Y = 459,596.119
 X = 845,757.186



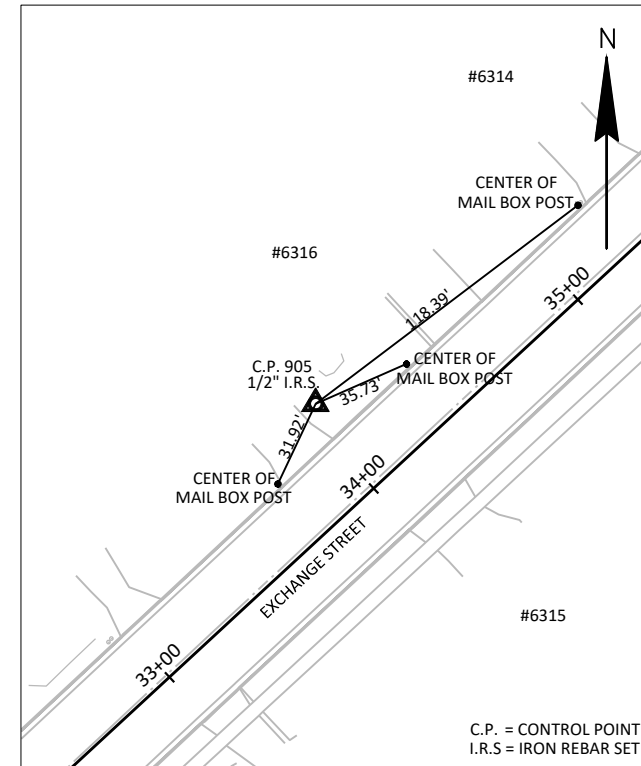
TIES TO C.P.#902
 STA. 44+80.24, 37.82' RT
 Y = 459,301.976
 X = 845,547.071



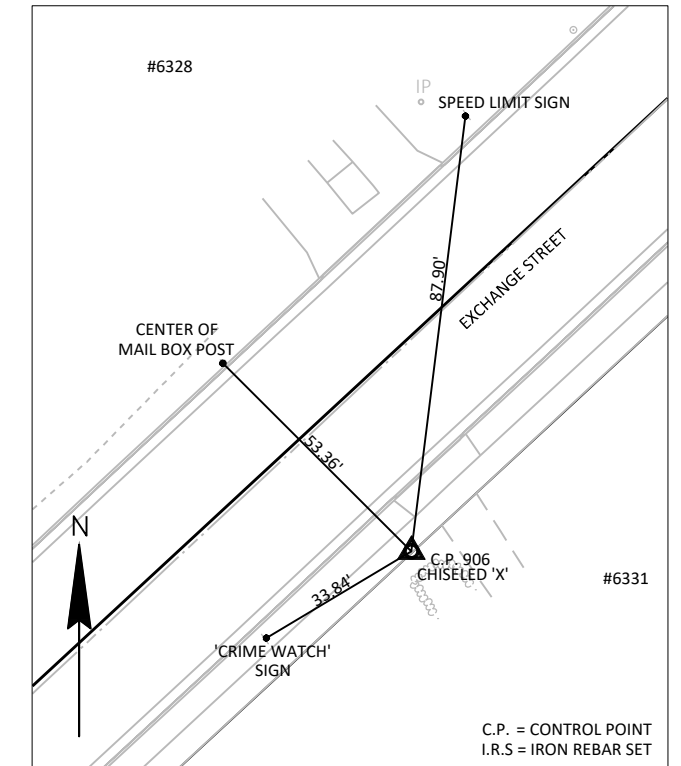
TIES TO C.P.#903
 STA. 41+49.29, 39.18' LT
 Y = 459,132.552
 X = 845,252.536



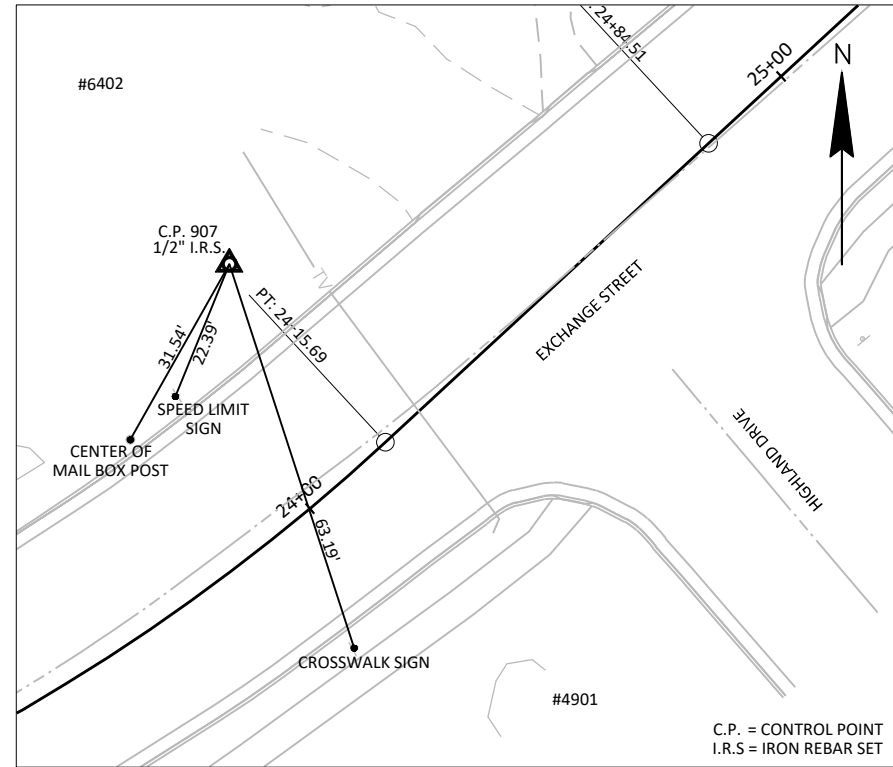
TIES TO C.P.#904
 STA. 37+72.75, 52.03' RT
 Y = 458,809.022
 X = 845,039.391



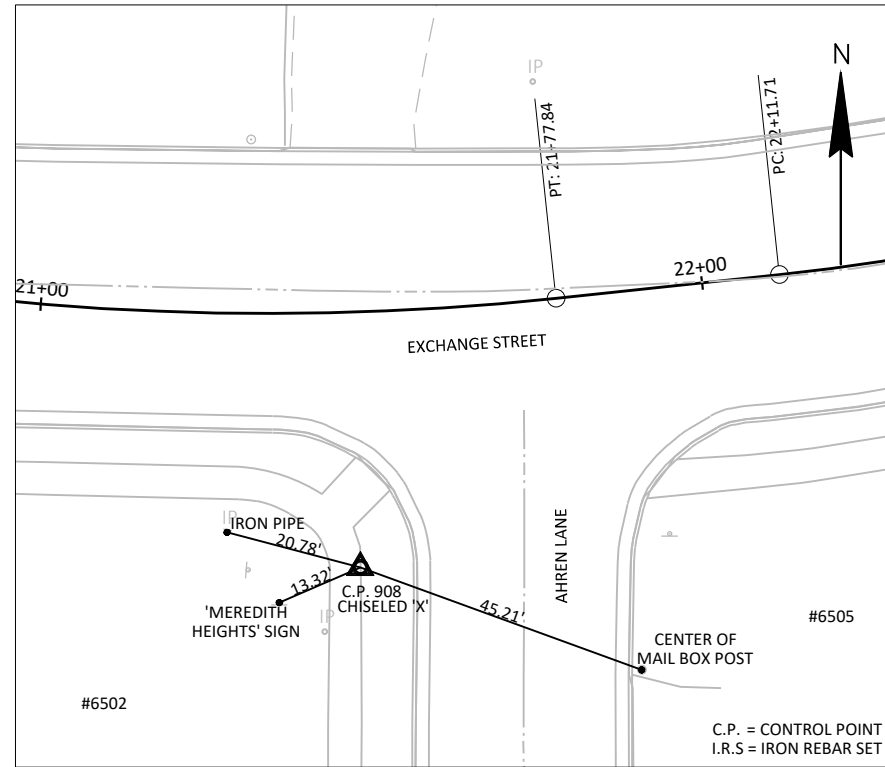
TIES TO C.P.#905
 STA. 34+05.27, 36.60' LT
 Y = 458,624.234
 X = 844,709.458



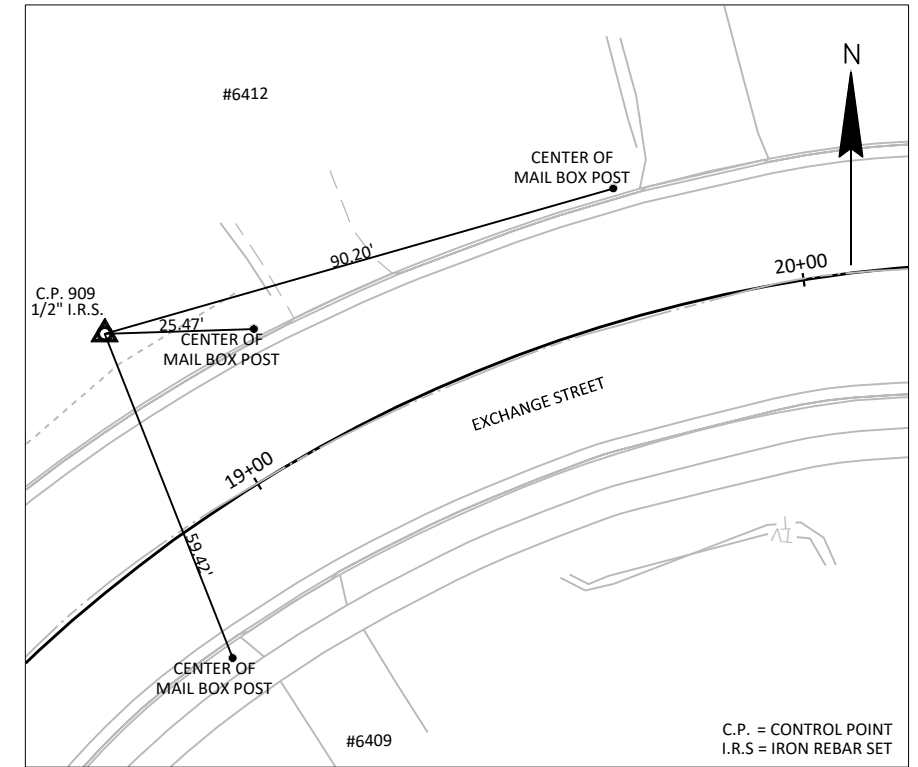
TIES TO C.P.#906
 STA. 28+49.96, 31.78' RT
 Y = 458,197.026
 X = 844,348.154



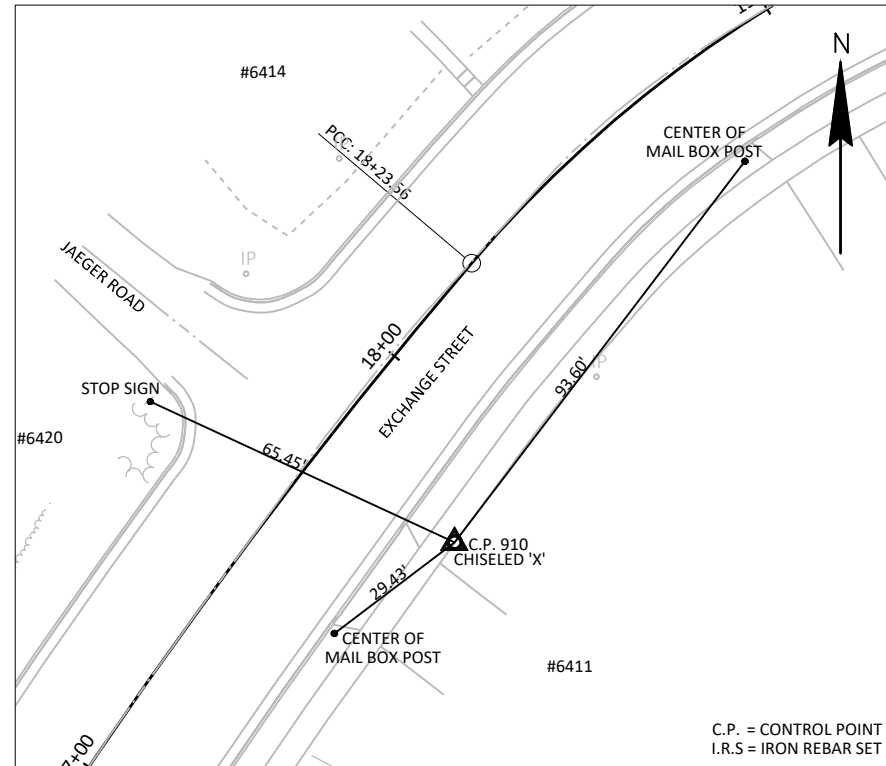
TIES TO C.P.#907
 STA. 24+16.71, 37.05' LT
 Y = 457,953.426
 X = 843,983.325



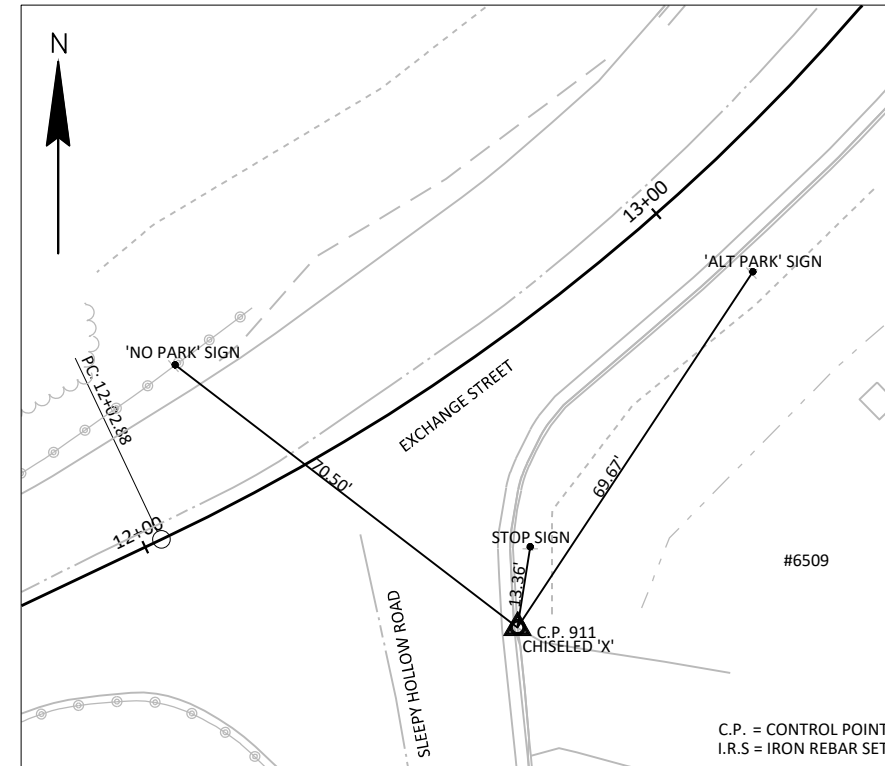
TIES TO C.P.#908
 STA. 21+47.04, 38.56' RT
 Y = 457,798.502
 X = 843,761.904



TIES TO C.P.#909
 STA. 18+92.40, 35.61' LT
 Y = 457,831.371
 X = 843,494.973

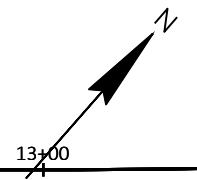


TIES TO C.P.#910
 STA. 17+78.56, 31.89' RT
 Y = 457,701.660
 X = 843,460.064



TIES TO C.P.#911
 STA. 12+44.55, 40.68' RT
 Y = 457,250.036
 X = 843,160.476

POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING	POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
1-1	12+71.48	33.77 RT	852.83	457272.59	843181.16	1-10	12+77.91	14.54 RT	853.87	457292.07	843175.02
1-2	12+76.03	33.34 RT	852.91	457275.94	843184.88	1-11	12+46.35	36.66 RT	853.65	457254.51	843160.02
1-3	12+75.64	28.36 RT	852.98	457279.64	843181.52	1-12	12+54.63	36.86 RT	853.19	457259.35	843167.79
1-4	12+71.05	28.79 RT	852.91	457276.31	843177.82	1-13	12+60.49	35.68 RT	853.12	457263.98	843172.46
1-5	12+70.34	21.00 RT	853.28	457282.12	843172.57	1-14	12+59.70	30.75 RT	853.18	457267.53	843168.95
1-6	12+75.05	21.03 RT	853.37	457285.09	843176.58	1-15	12+55.19	31.65 RT	853.26	457264.02	843165.39
1-7	12+66.35	14.56 RT	853.81	457284.85	843165.38	1-16	12+51.40	25.99 RT	853.71	457266.45	843158.84
1-8	12+69.72	14.50 RT	853.52	457286.96	843168.19	1-17	12+49.90	29.10 RT	853.37	457262.96	843159.19
1-9	12+74.55	14.53 RT	853.60	457289.95	843172.23	1-18	12+47.79	33.55 RT	853.34	457257.98	843159.68



EXCHANGE STREET

STA 12+84.63, 14.58' RT
HIGH POINT EL ???

- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATIONS, GRADES, SLOPES, LENGTHS, AND MATCH POINTS PRIOR TO THE CURB RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST CURB RAMP ELEVATIONS TO FIT FIELD CONDITIONS WITHIN THE REQUIREMENTS OF THE STANDARD DETAILS.
 3. DASHED SIDEWALK JOINT LINES SHOWN FOR PROPOSED SIDEWALK ARE FOR INFORMATIONAL PURPOSES ONLY AND DO NOT INDICATE EXACT JOINT LOCATIONS.
 4. SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS.
 5. ALL STATION AND OFFSET INFORMATION REFERENCE EXCHANGE STREET CENTERLINE ALIGNMENT.

SLEEPY HOLLOW ROAD

VILLAGE OF MCFARLAND

ALLIANT ENERGY

STA 12+44.52, 40.70' RT
HIGH POINT EL 853.63

SLOPE INTERCEPT

CURB RAMP DETAIL
EXCHANGE STREET/SLEEPY HOLLOW ROAD
(SE QUAD)

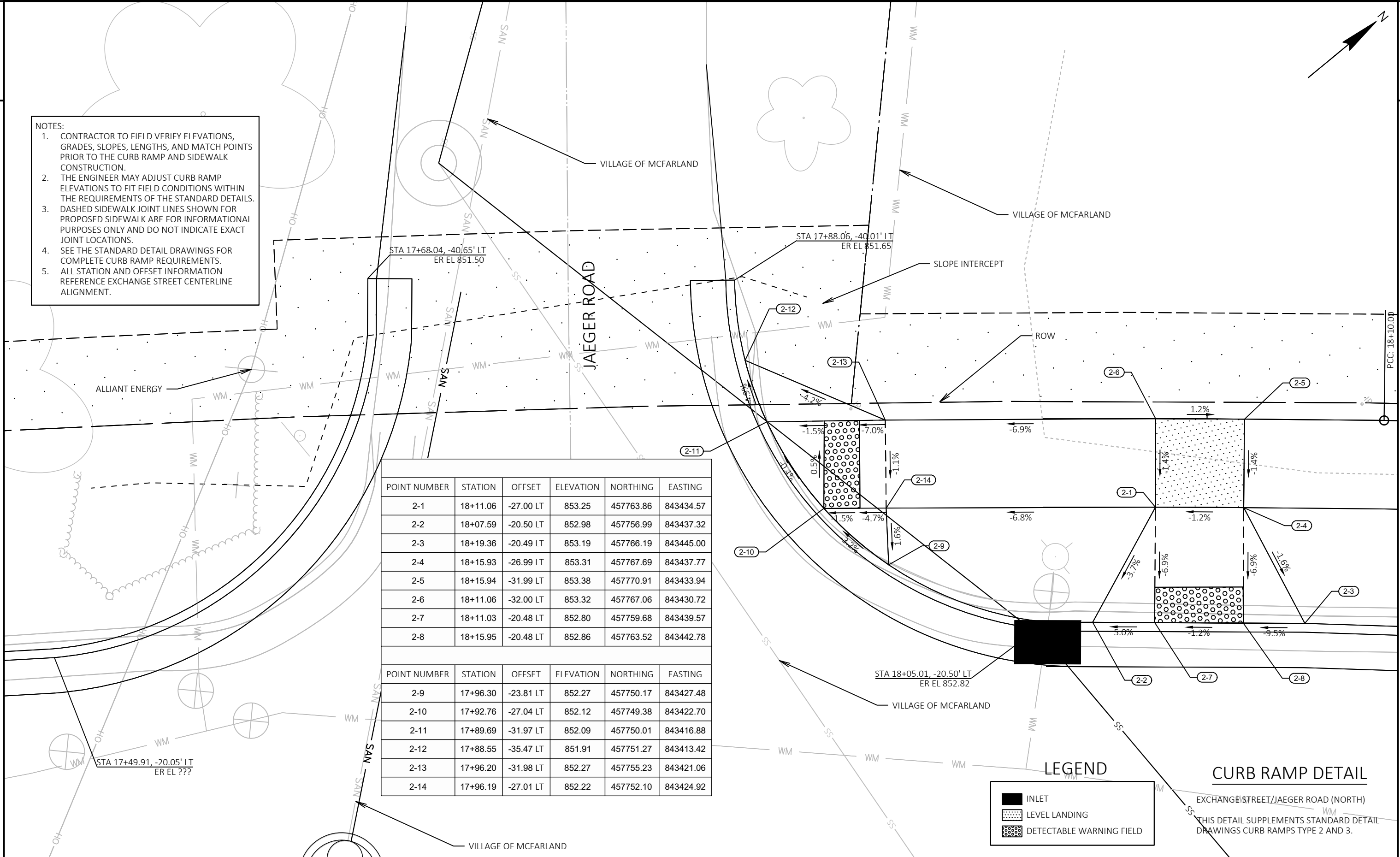
THIS DETAIL SUPPLEMENTS STANDARD DETAIL
DRAWINGS CURB RAMPS TYPE 2 AND 3.

LEGEND

- INLET
- LEVEL LANDING
- DETECTABLE WARNING FIELD

- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATIONS, GRADES, SLOPES, LENGTHS, AND MATCH POINTS PRIOR TO THE CURB RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST CURB RAMP ELEVATIONS TO FIT FIELD CONDITIONS WITHIN THE REQUIREMENTS OF THE STANDARD DETAILS.
 3. DASHED SIDEWALK JOINT LINES SHOWN FOR PROPOSED SIDEWALK ARE FOR INFORMATIONAL PURPOSES ONLY AND DO NOT INDICATE EXACT JOINT LOCATIONS.
 4. SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS.
 5. ALL STATION AND OFFSET INFORMATION REFERENCE EXCHANGE STREET CENTERLINE ALIGNMENT.

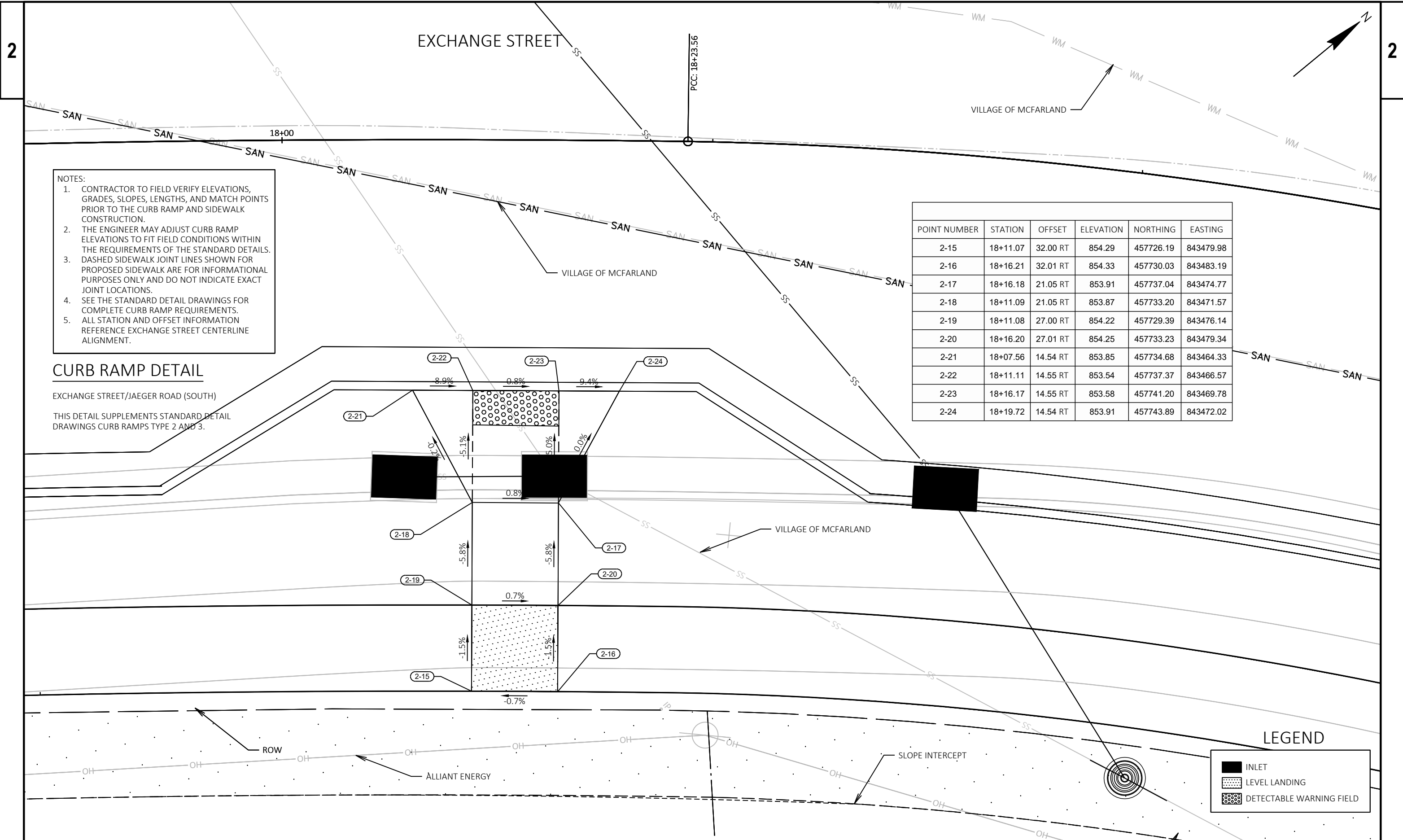
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
2-1	18+11.06	-27.00 LT	853.25	457763.86	843434.57
2-2	18+07.59	-20.50 LT	852.98	457756.99	843437.32
2-3	18+19.36	-20.49 LT	853.19	457766.19	843445.00
2-4	18+15.93	-26.99 LT	853.31	457767.69	843437.77
2-5	18+15.94	-31.99 LT	853.38	457770.91	843433.94
2-6	18+11.06	-32.00 LT	853.32	457767.06	843430.72
2-7	18+11.03	-20.48 LT	852.80	457759.68	843439.57
2-8	18+15.95	-20.48 LT	852.86	457763.52	843442.78
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
2-9	17+96.30	-23.81 LT	852.27	457750.17	843427.48
2-10	17+92.76	-27.04 LT	852.12	457749.38	843422.70
2-11	17+89.69	-31.97 LT	852.09	457750.01	843416.88
2-12	17+88.55	-35.47 LT	851.91	457751.27	843413.42
2-13	17+96.20	-31.98 LT	852.27	457755.23	843421.06
2-14	17+96.19	-27.01 LT	852.22	457752.10	843424.92



LEGEND

- INLET
- LEVEL LANDING
- DETECTABLE WARNING FIELD

CURB RAMP DETAIL
 EXCHANGE STREET/JAEGER ROAD (NORTH)
 THIS DETAIL SUPPLEMENTS STANDARD DETAIL DRAWINGS CURB RAMPS TYPE 2 AND 3.



- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATIONS, GRADES, SLOPES, LENGTHS, AND MATCH POINTS PRIOR TO THE CURB RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST CURB RAMP ELEVATIONS TO FIT FIELD CONDITIONS WITHIN THE REQUIREMENTS OF THE STANDARD DETAILS.
 3. DASHED SIDEWALK JOINT LINES SHOWN FOR PROPOSED SIDEWALK ARE FOR INFORMATIONAL PURPOSES ONLY AND DO NOT INDICATE EXACT JOINT LOCATIONS.
 4. SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS.
 5. ALL STATION AND OFFSET INFORMATION REFERENCE EXCHANGE STREET CENTERLINE ALIGNMENT.

CURB RAMP DETAIL

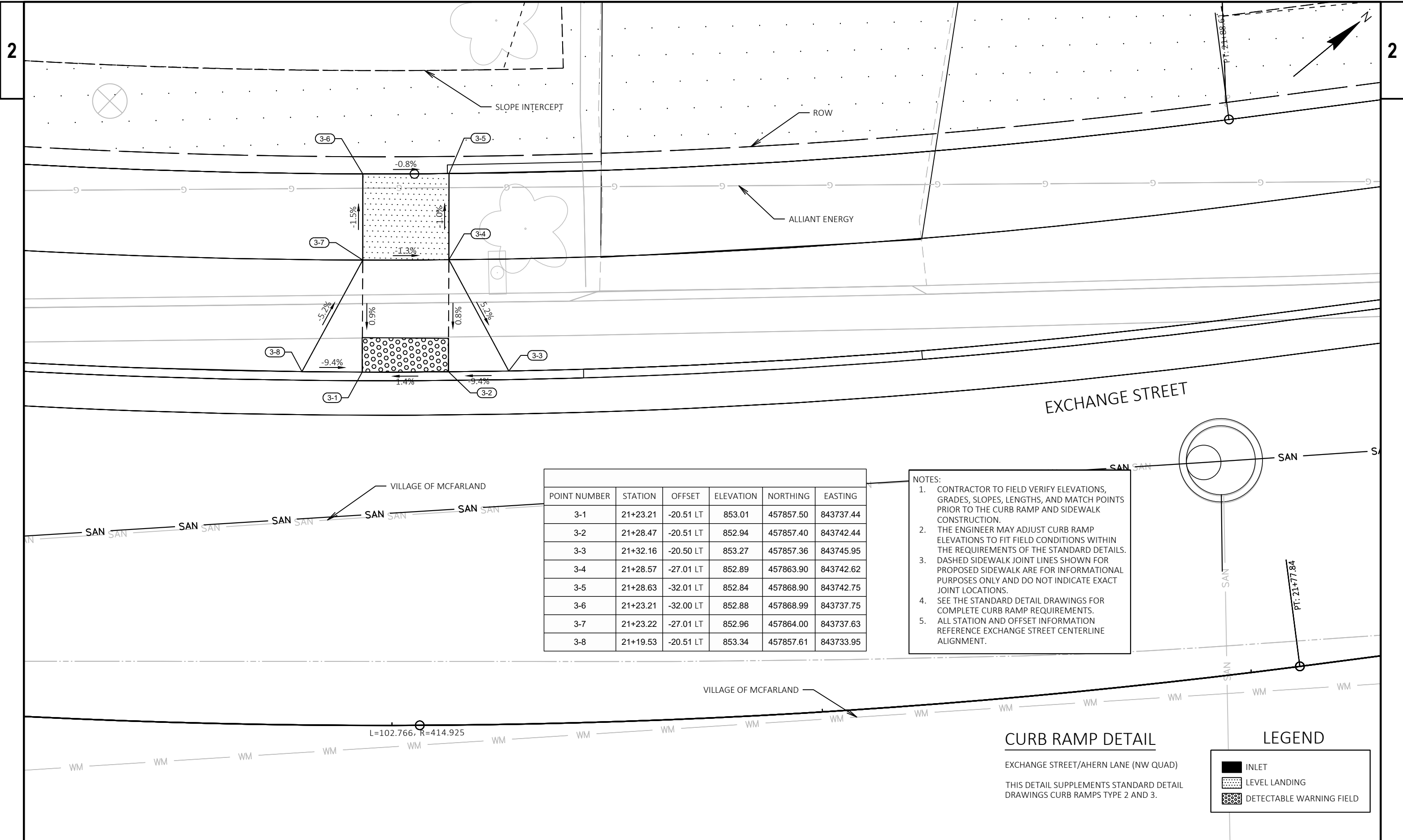
EXCHANGE STREET/JAEGER ROAD (SOUTH)

THIS DETAIL SUPPLEMENTS STANDARD DETAIL DRAWINGS CURB RAMPS TYPE 2 AND 3.

POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
2-15	18+11.07	32.00 RT	854.29	457726.19	843479.98
2-16	18+16.21	32.01 RT	854.33	457730.03	843483.19
2-17	18+16.18	21.05 RT	853.91	457737.04	843474.77
2-18	18+11.09	21.05 RT	853.87	457733.20	843471.57
2-19	18+11.08	27.00 RT	854.22	457729.39	843476.14
2-20	18+16.20	27.01 RT	854.25	457733.23	843479.34
2-21	18+07.56	14.54 RT	853.85	457734.68	843464.33
2-22	18+11.11	14.55 RT	853.54	457737.37	843466.57
2-23	18+16.17	14.55 RT	853.58	457741.20	843469.78
2-24	18+19.72	14.54 RT	853.91	457743.89	843472.02

LEGEND

	INLET
	LEVEL LANDING
	DETECTABLE WARNING FIELD



POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
3-1	21+23.21	-20.51 LT	853.01	457857.50	843737.44
3-2	21+28.47	-20.51 LT	852.94	457857.40	843742.44
3-3	21+32.16	-20.50 LT	853.27	457857.36	843745.95
3-4	21+28.57	-27.01 LT	852.89	457863.90	843742.62
3-5	21+28.63	-32.01 LT	852.84	457868.90	843742.75
3-6	21+23.21	-32.00 LT	852.88	457868.99	843737.75
3-7	21+23.22	-27.01 LT	852.96	457864.00	843737.63
3-8	21+19.53	-20.51 LT	853.34	457857.61	843733.95

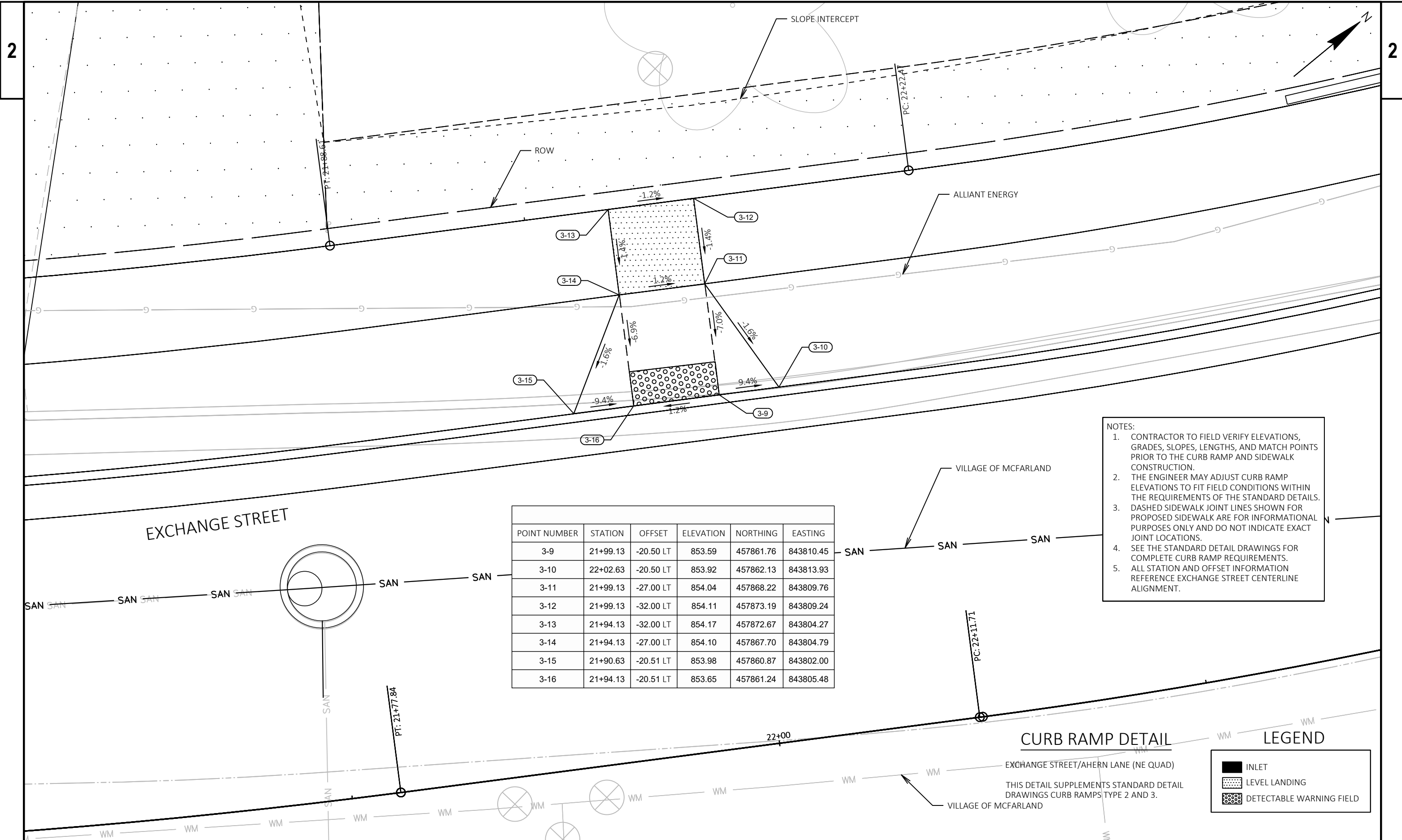
- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATIONS, GRADES, SLOPES, LENGTHS, AND MATCH POINTS PRIOR TO THE CURB RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST CURB RAMP ELEVATIONS TO FIT FIELD CONDITIONS WITHIN THE REQUIREMENTS OF THE STANDARD DETAILS.
 3. DASHED SIDEWALK JOINT LINES SHOWN FOR PROPOSED SIDEWALK ARE FOR INFORMATIONAL PURPOSES ONLY AND DO NOT INDICATE EXACT JOINT LOCATIONS.
 4. SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS.
 5. ALL STATION AND OFFSET INFORMATION REFERENCE EXCHANGE STREET CENTERLINE ALIGNMENT.

CURB RAMP DETAIL

EXCHANGE STREET/AHERN LANE (NW QUAD)
 THIS DETAIL SUPPLEMENTS STANDARD DETAIL DRAWINGS CURB RAMPS TYPE 2 AND 3.

LEGEND

- INLET
- LEVEL LANDING
- DETECTABLE WARNING FIELD



POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
3-9	21+99.13	-20.50 LT	853.59	457861.76	843810.45
3-10	22+02.63	-20.50 LT	853.92	457862.13	843813.93
3-11	21+99.13	-27.00 LT	854.04	457868.22	843809.76
3-12	21+99.13	-32.00 LT	854.11	457873.19	843809.24
3-13	21+94.13	-32.00 LT	854.17	457872.67	843804.27
3-14	21+94.13	-27.00 LT	854.10	457867.70	843804.79
3-15	21+90.63	-20.51 LT	853.98	457860.87	843802.00
3-16	21+94.13	-20.51 LT	853.65	457861.24	843805.48

- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATIONS, GRADES, SLOPES, LENGTHS, AND MATCH POINTS PRIOR TO THE CURB RAMP AND SIDEWALK CONSTRUCTION.
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 5. ALL STATION AND OFFSET INFORMATION REFERENCE EXCHANGE STREET CENTERLINE ALIGNMENT.

LEGEND

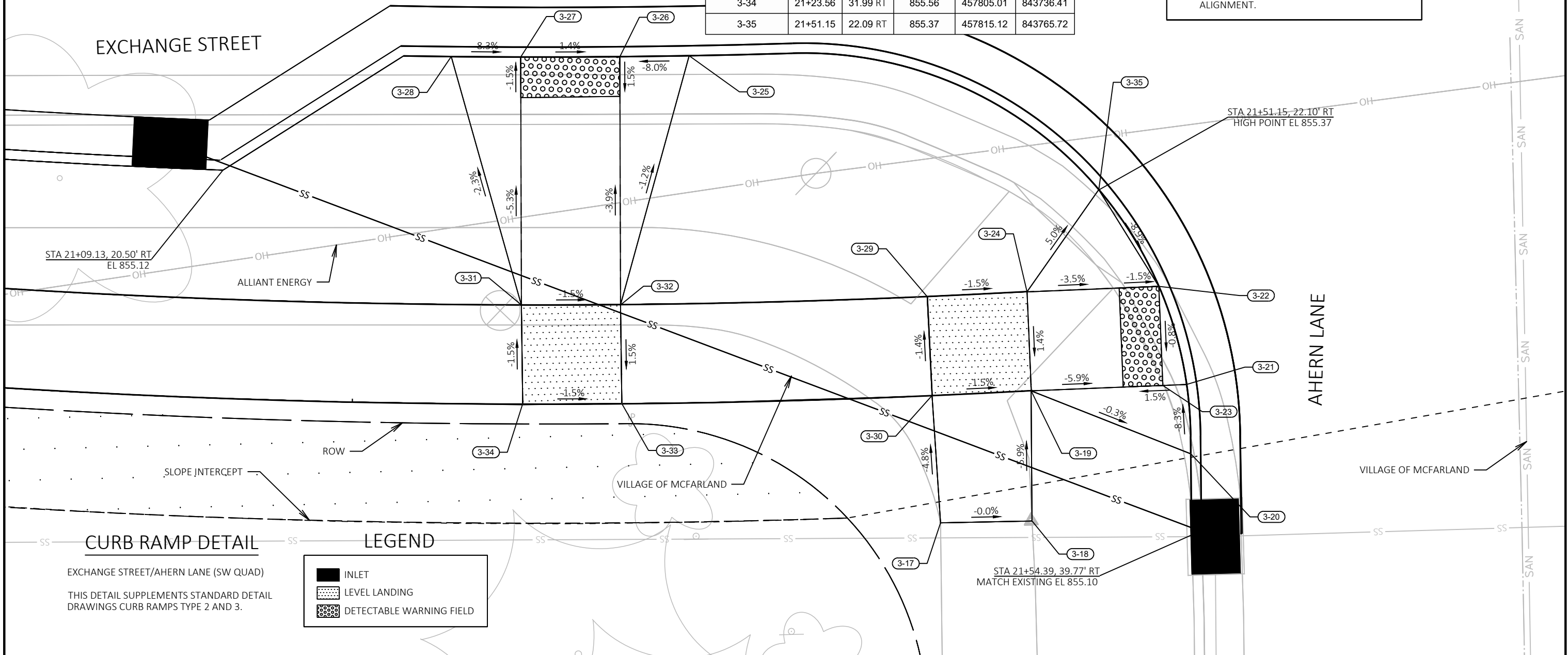
- INLET
- LEVEL LANDING
- DETECTABLE WARNING FIELD

VILLAGE OF MCFARLAND

POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
3-17	21+42.87	38.43 RT	855.51	457798.52	843757.34
3-18	21+47.09	38.56 RT	855.51	457798.51	843761.96
3-19	21+47.38	32.01 RT	855.13	457805.07	843762.06
3-20	21+54.62	35.69 RT	855.09	457801.70	843770.04
3-21	21+54.66	32.19 RT	854.80	457805.20	843769.91
3-22	21+53.69	27.15 RT	854.86	457810.18	843768.63
3-23	21+53.55	32.15 RT	854.82	457805.18	843768.71
3-24	21+47.45	27.01 RT	855.05	457810.07	843761.98
3-25	21+31.64	14.50 RT	855.25	457822.36	843745.23
3-26	21+28.26	14.50 RT	854.97	457822.40	843741.73

POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
3-27	21+23.43	14.50 RT	854.90	457822.50	843736.73
3-28	21+20.04	14.50 RT	855.19	457822.61	843733.23
3-29	21+42.71	27.00 RT	855.13	457809.94	843756.94
3-30	21+42.74	32.00 RT	855.20	457804.94	843757.07
3-31	21+23.49	26.99 RT	855.48	457810.01	843736.47
3-32	21+28.19	27.01 RT	855.41	457809.89	843741.47
3-33	21+28.20	31.99 RT	855.48	457804.91	843741.41
3-34	21+23.56	31.99 RT	855.56	457805.01	843736.41
3-35	21+51.15	22.09 RT	855.37	457815.12	843765.72

- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATIONS, GRADES, SLOPES, LENGTHS, AND MATCH POINTS PRIOR TO THE CURB RAMP AND SIDEWALK CONSTRUCTION.
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 3. DASHED SIDEWALK JOINT LINES SHOWN FOR PROPOSED SIDEWALK ARE FOR INFORMATIONAL PURPOSES ONLY AND DO NOT INDICATE EXACT JOINT LOCATIONS.
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 5. ALL STATION AND OFFSET INFORMATION REFERENCE EXCHANGE STREET CENTERLINE ALIGNMENT.

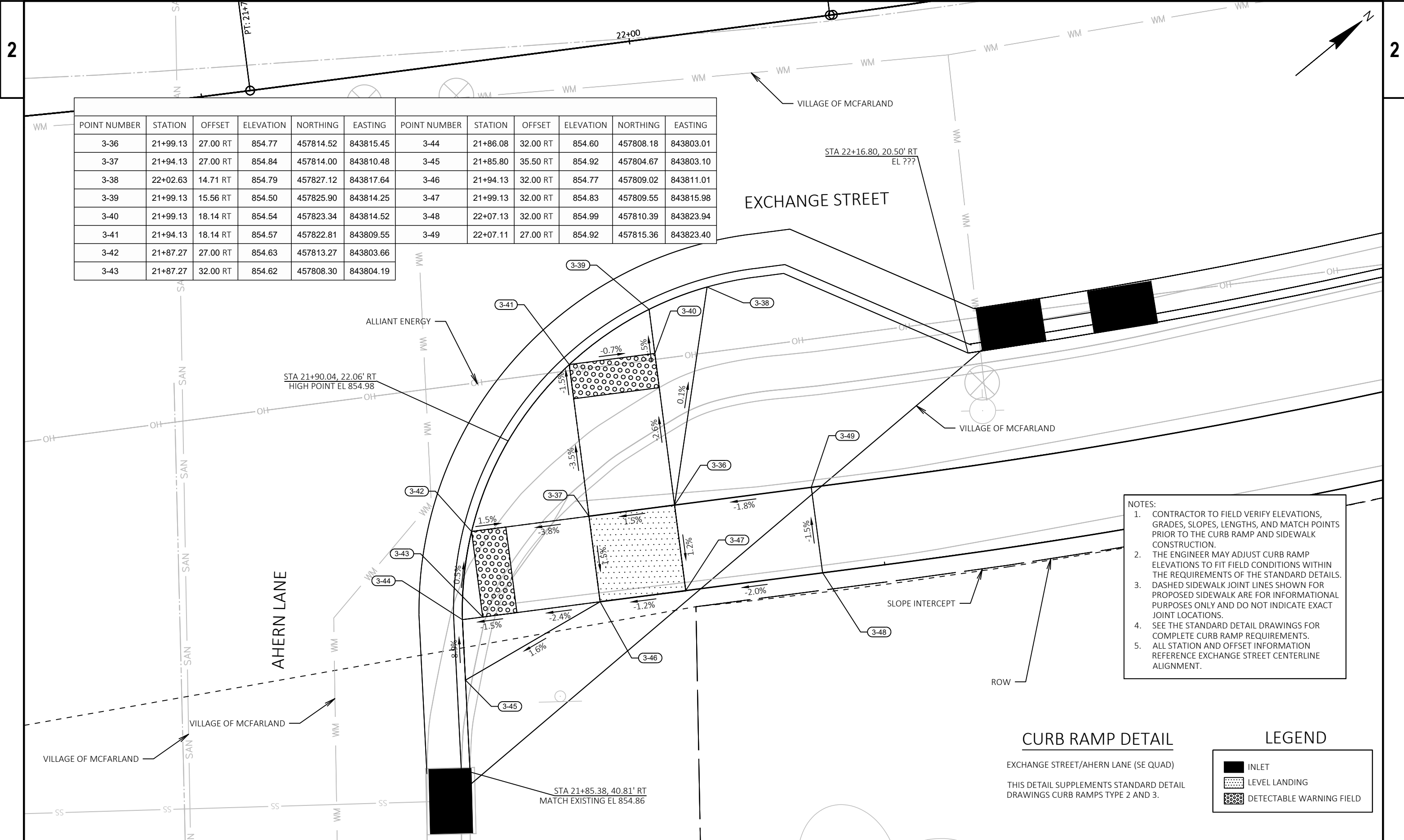


CURB RAMP DETAIL

LEGEND

EXCHANGE STREET/AHERN LANE (SW QUAD)
 THIS DETAIL SUPPLEMENTS STANDARD DETAIL DRAWINGS CURB RAMPS TYPE 2 AND 3.

INLET
 LEVEL LANDING
 DETECTABLE WARNING FIELD



POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING	POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
3-36	21+99.13	27.00 RT	854.77	457814.52	843815.45	3-44	21+86.08	32.00 RT	854.60	457808.18	843803.01
3-37	21+94.13	27.00 RT	854.84	457814.00	843810.48	3-45	21+85.80	35.50 RT	854.92	457804.67	843803.10
3-38	22+02.63	14.71 RT	854.79	457827.12	843817.64	3-46	21+94.13	32.00 RT	854.77	457809.02	843811.01
3-39	21+99.13	15.56 RT	854.50	457825.90	843814.25	3-47	21+99.13	32.00 RT	854.83	457809.55	843815.98
3-40	21+99.13	18.14 RT	854.54	457823.34	843814.52	3-48	22+07.13	32.00 RT	854.99	457810.39	843823.94
3-41	21+94.13	18.14 RT	854.57	457822.81	843809.55	3-49	22+07.11	27.00 RT	854.92	457815.36	843823.40
3-42	21+87.27	27.00 RT	854.63	457813.27	843803.66						
3-43	21+87.27	32.00 RT	854.62	457808.30	843804.19						

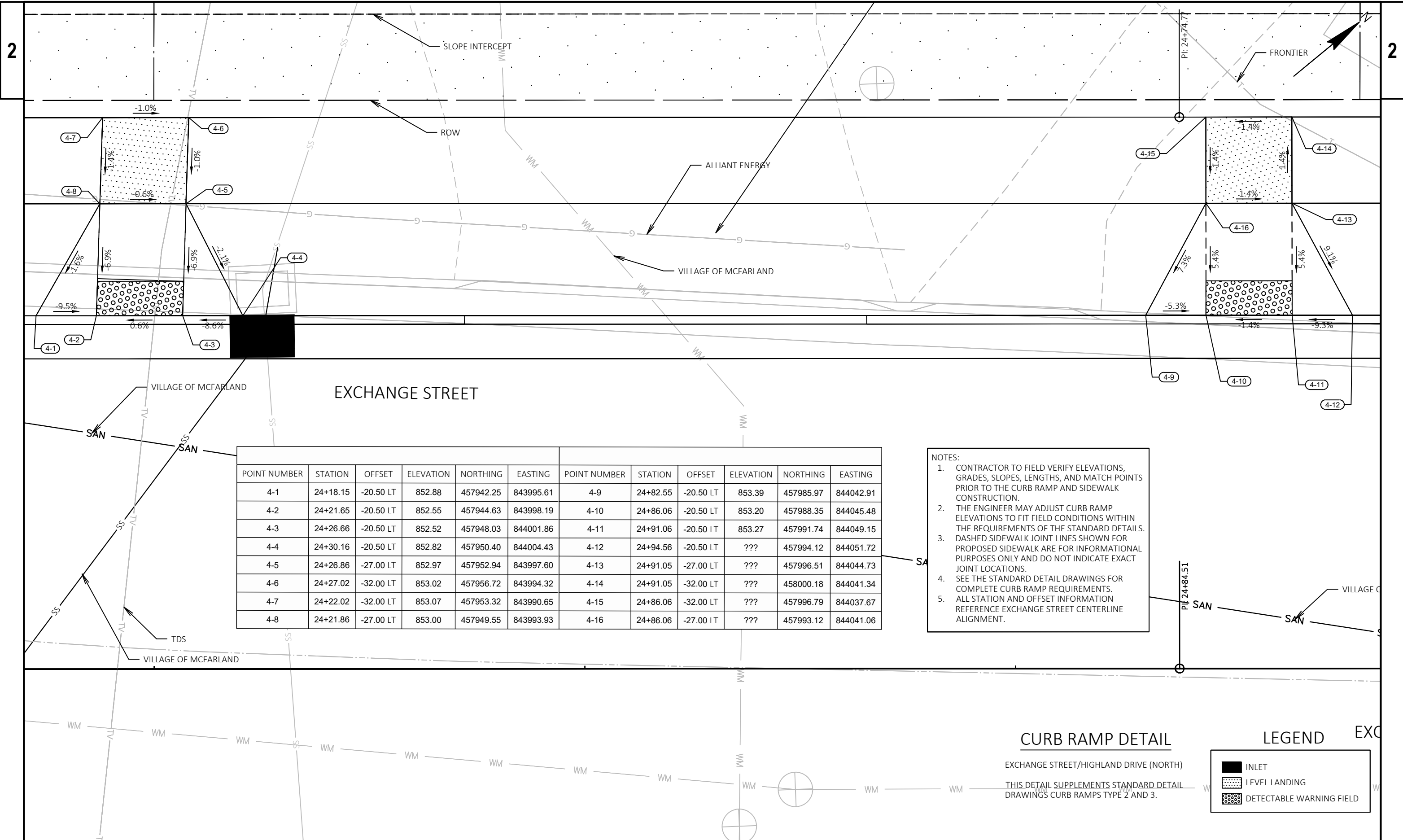
- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATIONS, GRADES, SLOPES, LENGTHS, AND MATCH POINTS PRIOR TO THE CURB RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST CURB RAMP ELEVATIONS TO FIT FIELD CONDITIONS WITHIN THE REQUIREMENTS OF THE STANDARD DETAILS.
 3. DASHED SIDEWALK JOINT LINES SHOWN FOR PROPOSED SIDEWALK ARE FOR INFORMATIONAL PURPOSES ONLY AND DO NOT INDICATE EXACT JOINT LOCATIONS.
 4. SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS.
 5. ALL STATION AND OFFSET INFORMATION REFERENCE EXCHANGE STREET CENTERLINE ALIGNMENT.

CURB RAMP DETAIL

EXCHANGE STREET/AHERN LANE (SE QUAD)
 THIS DETAIL SUPPLEMENTS STANDARD DETAIL DRAWINGS CURB RAMPS TYPE 2 AND 3.

LEGEND

- INLET
- LEVEL LANDING
- DETECTABLE WARNING FIELD



POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING	POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
4-1	24+18.15	-20.50 LT	852.88	457942.25	843995.61	4-9	24+82.55	-20.50 LT	853.39	457985.97	844042.91
4-2	24+21.65	-20.50 LT	852.55	457944.63	843998.19	4-10	24+86.06	-20.50 LT	853.20	457988.35	844045.48
4-3	24+26.66	-20.50 LT	852.52	457948.03	844001.86	4-11	24+91.06	-20.50 LT	853.27	457991.74	844049.15
4-4	24+30.16	-20.50 LT	852.82	457950.40	844004.43	4-12	24+94.56	-20.50 LT	???	457994.12	844051.72
4-5	24+26.86	-27.00 LT	852.97	457952.94	843997.60	4-13	24+91.05	-27.00 LT	???	457996.51	844044.73
4-6	24+27.02	-32.00 LT	853.02	457956.72	843994.32	4-14	24+91.05	-32.00 LT	???	458000.18	844041.34
4-7	24+22.02	-32.00 LT	853.07	457953.32	843990.65	4-15	24+86.06	-32.00 LT	???	457996.79	844037.67
4-8	24+21.86	-27.00 LT	853.00	457949.55	843993.93	4-16	24+86.06	-27.00 LT	???	457993.12	844041.06

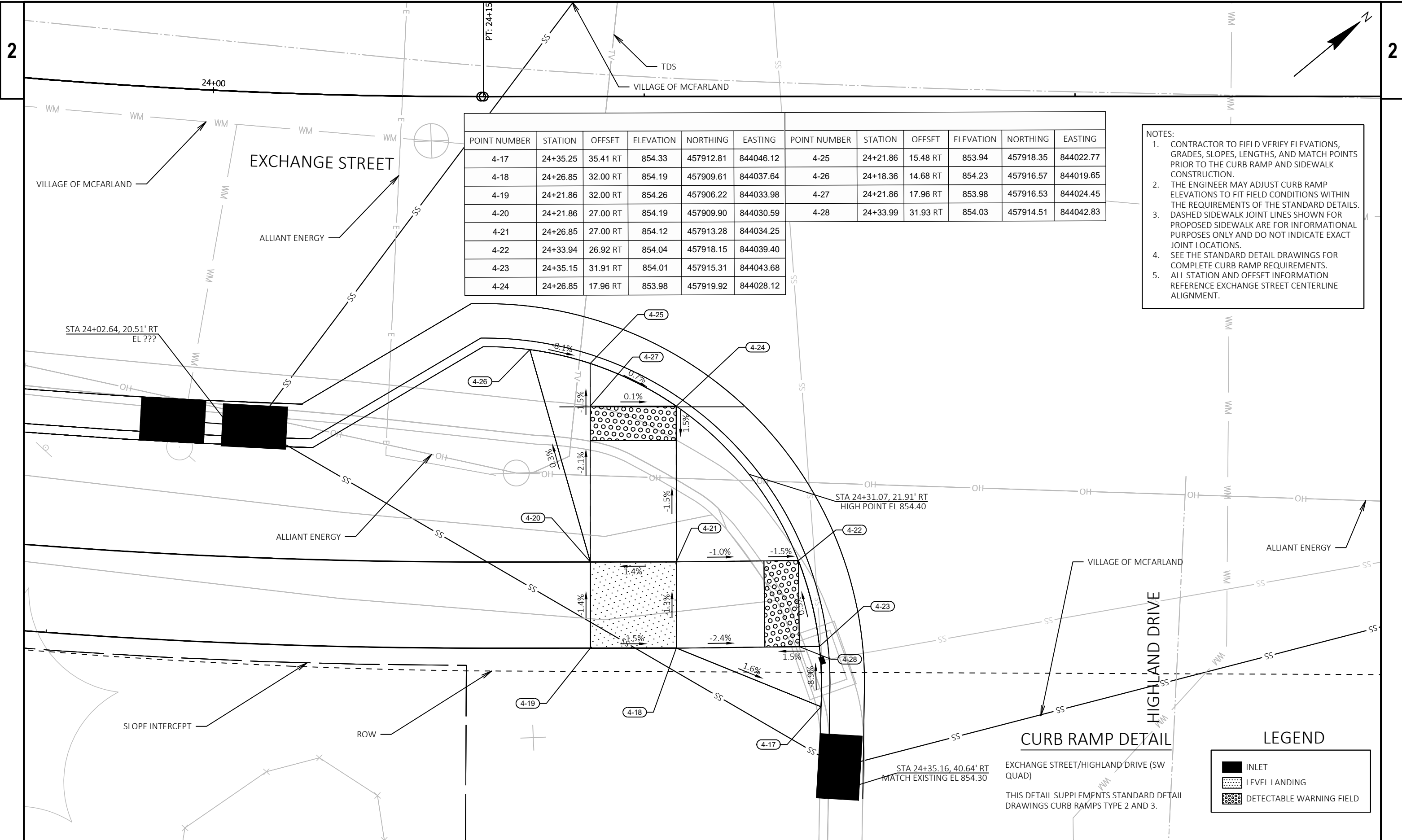
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 - ALL STATION AND OFFSET INFORMATION REFERENCE EXCHANGE STREET CENTERLINE ALIGNMENT.

CURB RAMP DETAIL

EXCHANGE STREET/HIGHLAND DRIVE (NORTH)
 THIS DETAIL SUPPLEMENTS STANDARD DETAIL DRAWINGS CURB RAMPS TYPE 2 AND 3.

LEGEND

- INLET
- LEVEL LANDING
- DETECTABLE WARNING FIELD



POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING	POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
4-17	24+35.25	35.41 RT	854.33	457912.81	844046.12	4-25	24+21.86	15.48 RT	853.94	457918.35	844022.77
4-18	24+26.85	32.00 RT	854.19	457909.61	844037.64	4-26	24+18.36	14.68 RT	854.23	457916.57	844019.65
4-19	24+21.86	32.00 RT	854.26	457906.22	844033.98	4-27	24+21.86	17.96 RT	853.98	457916.53	844024.45
4-20	24+21.86	27.00 RT	854.19	457909.90	844030.59	4-28	24+33.99	31.93 RT	854.03	457914.51	844042.83
4-21	24+26.85	27.00 RT	854.12	457913.28	844034.25						
4-22	24+33.94	26.92 RT	854.04	457918.15	844039.40						
4-23	24+35.15	31.91 RT	854.01	457915.31	844043.68						
4-24	24+26.85	17.96 RT	853.98	457919.92	844028.12						

- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATIONS, GRADES, SLOPES, LENGTHS, AND MATCH POINTS PRIOR TO THE CURB RAMP AND SIDEWALK CONSTRUCTION.
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LEGEND

- INLET
- LEVEL LANDING
- DETECTABLE WARNING FIELD

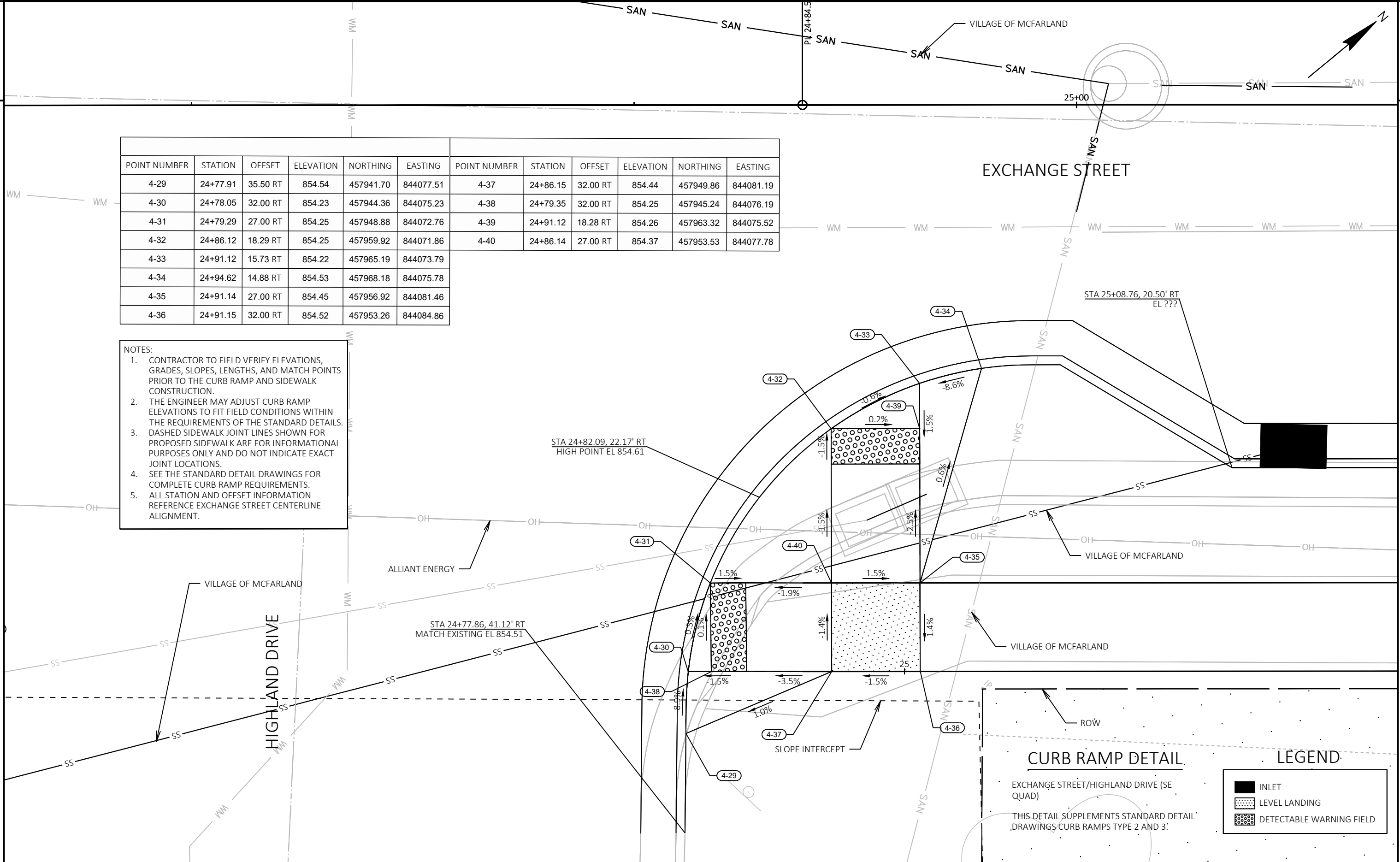
CURB RAMP DETAIL

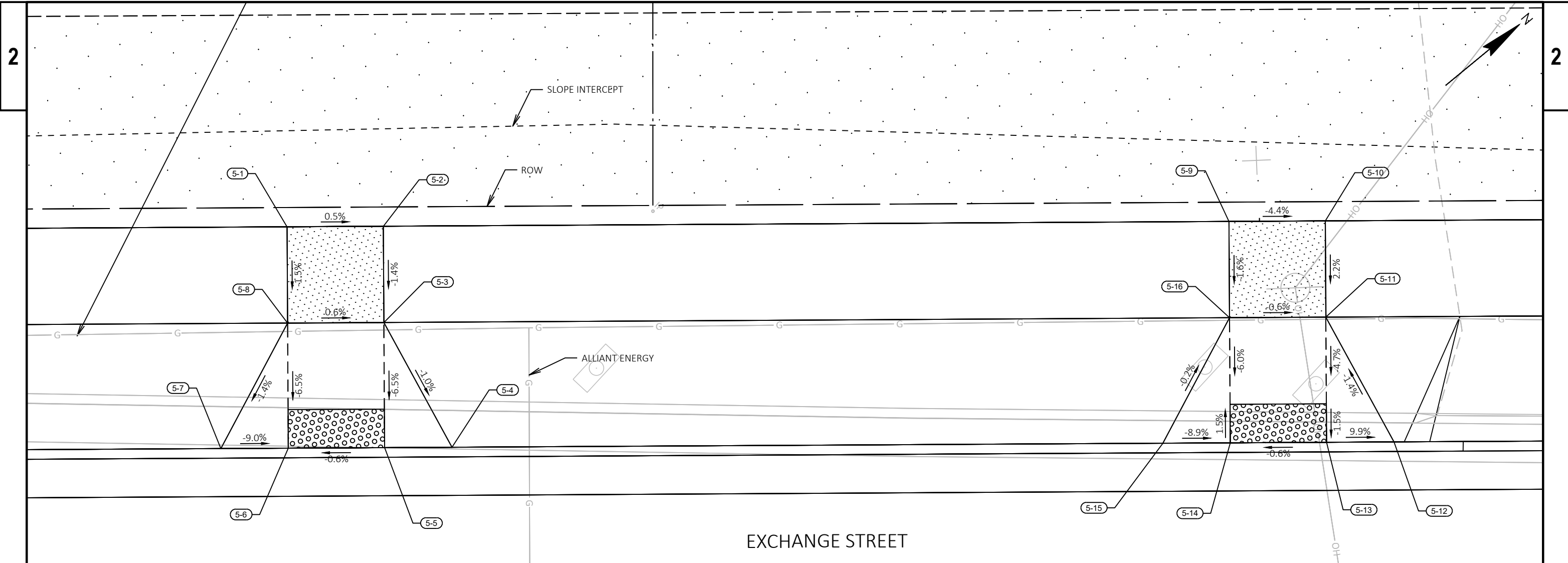
EXCHANGE STREET/HIGHLAND DRIVE (SW QUAD)

THIS DETAIL SUPPLEMENTS STANDARD DETAIL DRAWINGS CURB RAMPS TYPE 2 AND 3.

POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING	POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
4-29	24+77.91	35.50 RT	854.54	457941.70	844077.51	4-37	24+86.15	32.00 RT	854.44	457949.86	844081.19
4-30	24+78.05	32.00 RT	854.23	457944.36	844075.23	4-38	24+79.35	32.00 RT	854.25	457945.24	844076.19
4-31	24+79.29	27.00 RT	854.25	457948.88	844072.76	4-39	24+91.12	18.28 RT	854.26	457963.32	844075.52
4-32	24+86.12	18.29 RT	854.25	457959.92	844071.86	4-40	24+86.14	27.00 RT	854.37	457953.53	844077.78
4-33	24+91.12	15.73 RT	854.22	457965.19	844073.79						
4-34	24+94.62	14.88 RT	854.53	457968.18	844075.78						
4-35	24+91.14	27.00 RT	854.45	457956.92	844081.46						
4-36	24+91.15	32.00 RT	854.52	457953.26	844084.86						

- NOTES:
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EXCHANGE STREET

POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING	POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
5-1	37+59.38	-32.00 LT	888.36	458861.36	844972.30	5-9	38+08.30	-32.00 LT	888.51	458894.72	845008.07
5-2	37+64.38	-32.00 LT	888.39	458864.77	844975.96	5-10	38+13.30	-32.00 LT	888.29	458898.13	845011.73
5-3	37+64.38	-27.00 LT	888.32	458861.11	844979.37	5-11	38+13.30	-27.00 LT	888.40	458894.47	845015.14
5-4	37+67.88	-20.50 LT	888.25	458858.74	844986.36	5-12	38+16.80	-20.50 LT	888.51	458892.11	845022.13
5-5	37+64.38	-20.50 LT	887.90	458856.36	844983.80	5-13	38+13.30	-20.50 LT	888.16	458889.72	845019.57
5-6	37+59.38	-20.50 LT	887.87	458852.95	844980.14	5-14	38+08.30	-20.50 LT	888.13	458886.31	845015.92
5-7	37+55.88	-20.50 LT	888.18	458850.56	844977.58	5-15	38+04.80	-20.50 LT	888.44	458883.93	845013.36
5-8	37+59.38	-27.00 LT	888.29	458857.70	844975.71	5-16	38+08.30	-27.00 LT	888.43	458891.07	845011.48

- NOTES:
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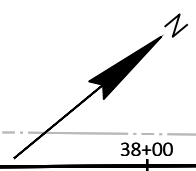
CURB RAMP DETAIL

EXCHANGE STREET/LEWIS LANE (NORTH)
 THIS DETAIL SUPPLEMENTS STANDARD DETAIL DRAWINGS CURB RAMPS TYPE 2 AND 3.

LEGEND

- INLET
- ▨ LEVEL LANDING
- ▩ DETECTABLE WARNING FIELD

38+00

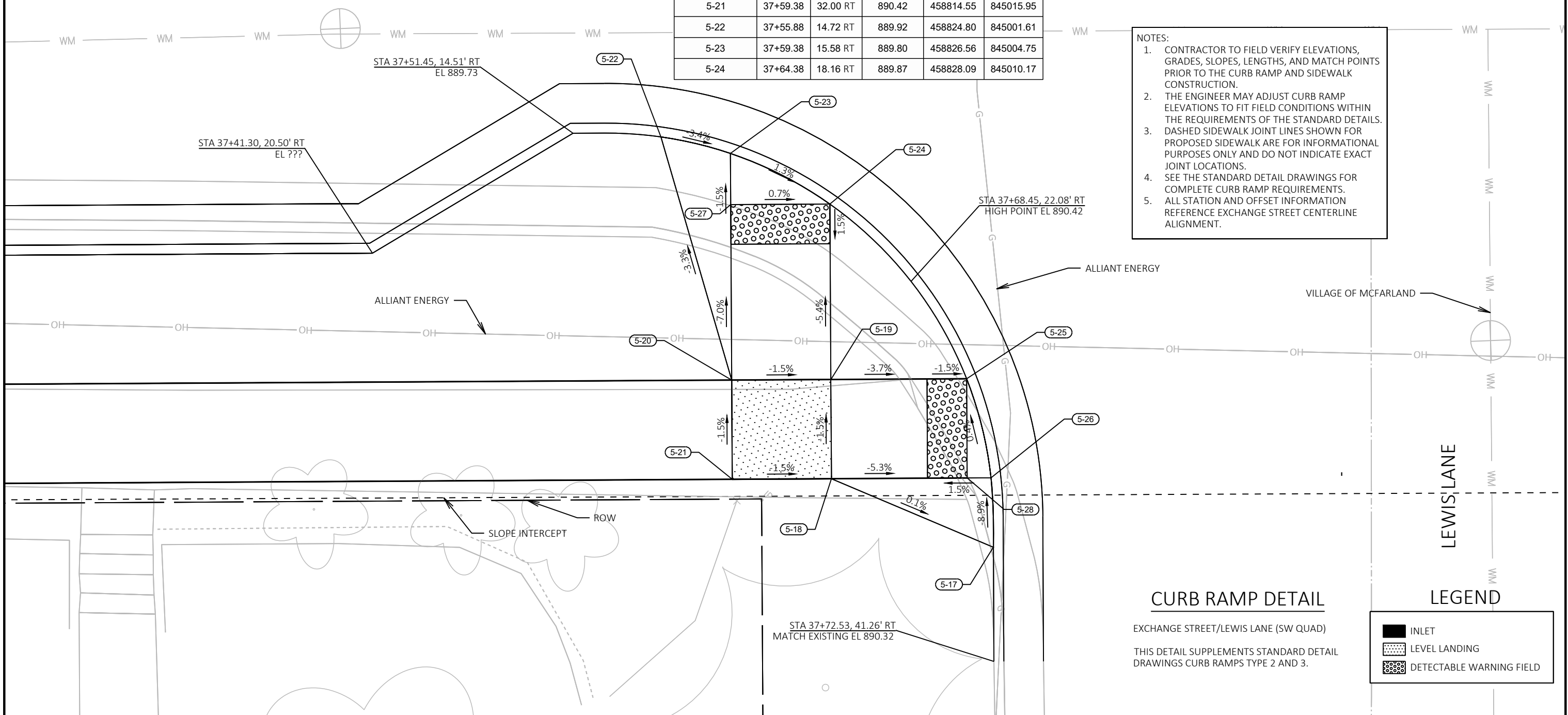


38+00

EXCHANGE STREET

POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING	POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
5-17	37+72.53	35.50 RT	890.35	458820.96	845027.96	5-25	37+71.23	27.00 RT	890.06	458826.29	845021.21
5-18	37+64.38	32.00 RT	890.34	458817.96	845019.61	5-26	37+72.43	32.00 RT	890.04	458823.45	845025.50
5-19	37+64.38	27.00 RT	890.27	458821.62	845016.20	5-27	37+59.38	18.16 RT	889.84	458824.68	845006.51
5-20	37+59.38	27.00 RT	890.34	458818.21	845012.54	5-28	37+71.23	32.00 RT	890.06	458822.64	845024.62
5-21	37+59.38	32.00 RT	890.42	458814.55	845015.95						
5-22	37+55.88	14.72 RT	889.92	458824.80	845001.61						
5-23	37+59.38	15.58 RT	889.80	458826.56	845004.75						
5-24	37+64.38	18.16 RT	889.87	458828.09	845010.17						

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LEGEND

- INLET
- LEVEL LANDING
- DETECTABLE WARNING FIELD

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POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING	POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
5-29	38+20.94	27.00 RT	889.92	458860.20	845057.56	5-37	38+08.99	32.00 RT	889.73	458848.39	845052.23
5-30	38+24.44	14.69 RT	889.65	458871.59	845051.73	5-38	38+07.70	35.50 RT	890.06	458844.95	845053.67
5-31	38+20.94	15.53 RT	889.37	458868.59	845049.74	5-39	38+15.94	32.00 RT	889.95	458853.13	845057.32
5-32	38+15.94	18.07 RT	889.45	458863.32	845047.82	5-40	38+20.94	32.00 RT	890.00	458856.54	845060.97
5-33	38+08.99	27.00 RT	889.66	458852.05	845048.82						
5-34	38+15.94	27.00 RT	889.87	458856.79	845053.91						
5-35	38+07.80	32.00 RT	889.72	458847.58	845051.36						
5-36	38+20.94	18.07 RT	889.41	458866.73	845051.47						

EXCHANGE STREET

VILLAGE OF MCFARLAND

STA 38+39.03, 20.50' RT
EL 889.42

VILLAGE OF MCFARLAND

LEWIS LANE




STA 38+07.73, 42.24' RT
MATCH EXISTING EL 890.16

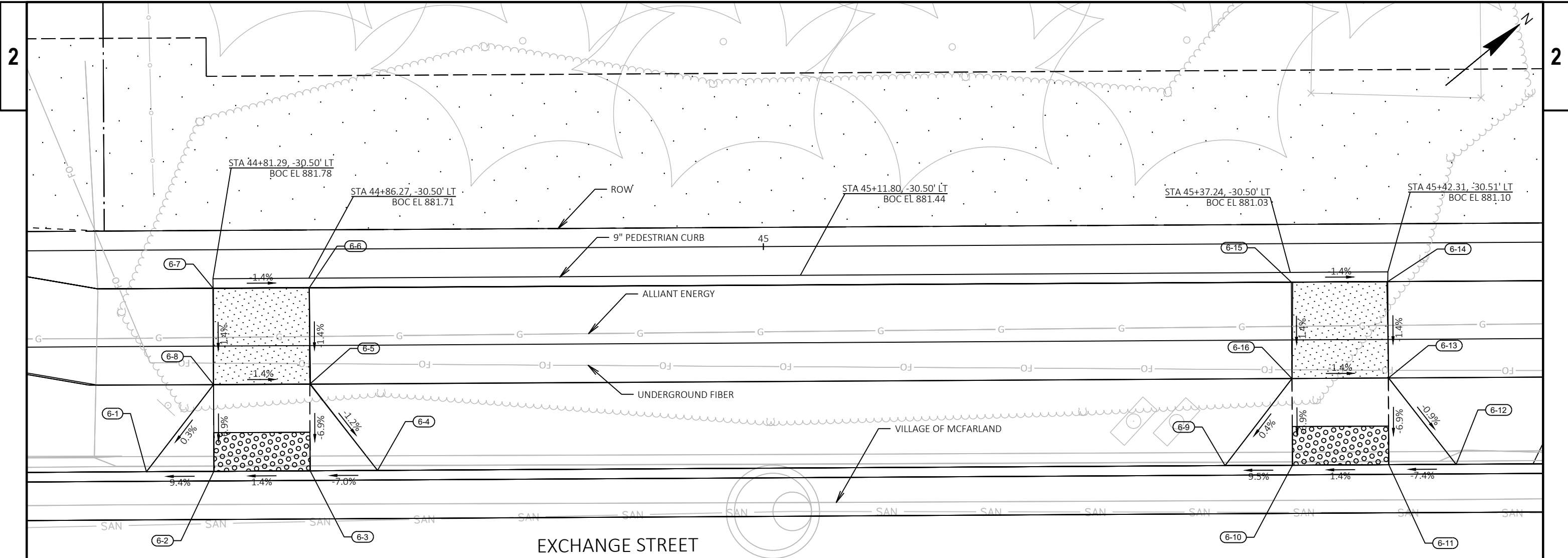
ALLIANT ENERGY

CURB RAMP DETAIL

EXCHANGE STREET/LEWIS LANE (SE QUAD)
THIS DETAIL SUPPLEMENTS STANDARD DETAIL DRAWINGS CURB RAMPS TYPE 2 AND 3.

LEGEND

-  INLET
-  LEVEL LANDING
-  DETECTABLE WARNING FIELD



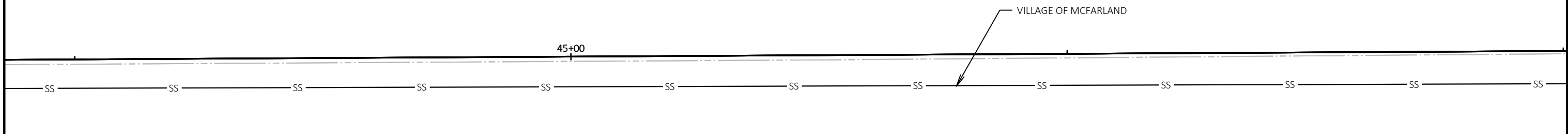
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING	POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
6-1	44+77.78	-20.50 LT	880.98	459342.95	845505.49	6-9	45+33.79	-20.50 LT	880.30	459381.16	845546.45
6-2	44+81.28	-20.50 LT	880.65	459345.34	845508.05	6-10	45+37.29	-20.50 LT	879.97	459383.54	845549.01
6-3	44+86.28	-20.50 LT	880.58	459348.75	845511.71	6-11	45+42.29	-20.50 LT	879.90	459386.95	845552.67
6-4	44+89.78	-20.50 LT	880.82	459351.14	845514.27	6-12	45+45.79	-20.50 LT	880.16	459389.34	845555.23
6-5	44+86.29	-25.00 LT	880.89	459352.05	845508.65	6-13	45+42.31	-25.00 LT	880.21	459390.25	845549.61
6-6	44+86.29	-30.00 LT	880.96	459355.71	845505.23	6-14	45+42.30	-30.00 LT	880.28	459393.90	845546.19
6-7	44+81.30	-30.00 LT	881.03	459352.30	845501.58	6-15	45+37.29	-30.00 LT	880.35	459390.49	845542.53
6-8	44+81.29	-25.00 LT	880.96	459348.63	845504.99	6-16	45+37.29	-25.00 LT	880.28	459386.83	845545.94

- NOTES:
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LEGEND

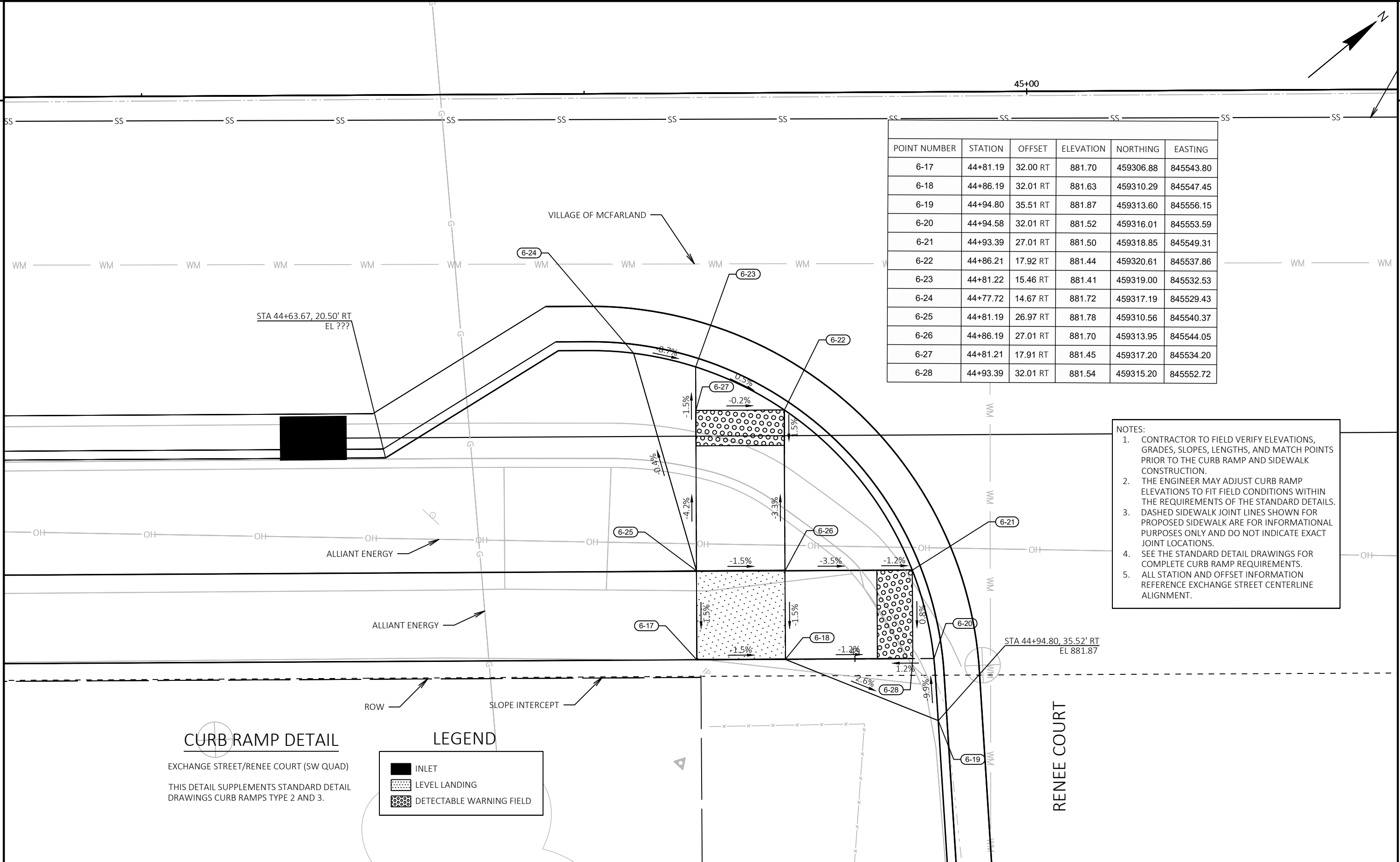
- INLET
- LEVEL LANDING
- DETECTABLE WARNING FIELD

CURB RAMP DETAIL
EXCHANGE STREET/RENEE COURT (NORTH)
THIS DETAIL SUPPLEMENTS STANDARD DETAIL DRAWINGS CURB RAMPS TYPE 2 AND 3.



45+00

POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
6-17	44+81.19	32.00 RT	881.70	459306.88	845543.80
6-18	44+86.19	32.01 RT	881.63	459310.29	845547.45
6-19	44+94.80	35.51 RT	881.87	459313.60	845556.15
6-20	44+94.58	32.01 RT	881.52	459316.01	845553.59
6-21	44+93.39	27.01 RT	881.50	459318.85	845549.31
6-22	44+86.21	17.92 RT	881.44	459320.61	845537.86
6-23	44+81.22	15.46 RT	881.41	459319.00	845532.53
6-24	44+77.72	14.67 RT	881.72	459317.19	845529.43
6-25	44+81.19	26.97 RT	881.78	459310.56	845540.37
6-26	44+86.19	27.01 RT	881.70	459313.95	845544.05
6-27	44+81.21	17.91 RT	881.45	459317.20	845534.20
6-28	44+93.39	32.01 RT	881.54	459315.20	845552.72



STA 44+63.67, 20.50' RT
EL ???

STA 44+94.80, 35.52' RT
EL 881.87

- NOTES:
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CURB RAMP DETAIL

EXCHANGE STREET/RENEE COURT (SW QUAD)
 THIS DETAIL SUPPLEMENTS STANDARD DETAIL DRAWINGS CURB RAMPS TYPE 2 AND 3.

LEGEND

- INLET
- LEVEL LANDING
- DETECTABLE WARNING FIELD

- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATIONS, GRADES, SLOPES, LENGTHS, AND MATCH POINTS PRIOR TO THE CURB RAMP AND SIDEWALK CONSTRUCTION.
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EXCHANGE STREET

VILLAGE OF MCFARLAND

ALLIANT ENERGY

ALLIANT ENERGY

SLOPE INTERCEPT

ROW

RENEE COURT

STA 45+29.29, 49.10' RT
EL 881.77

STA 45+50.44, 14.46' RT
EL 881.06




STA 45+60.39, 20.50' RT
EL ???

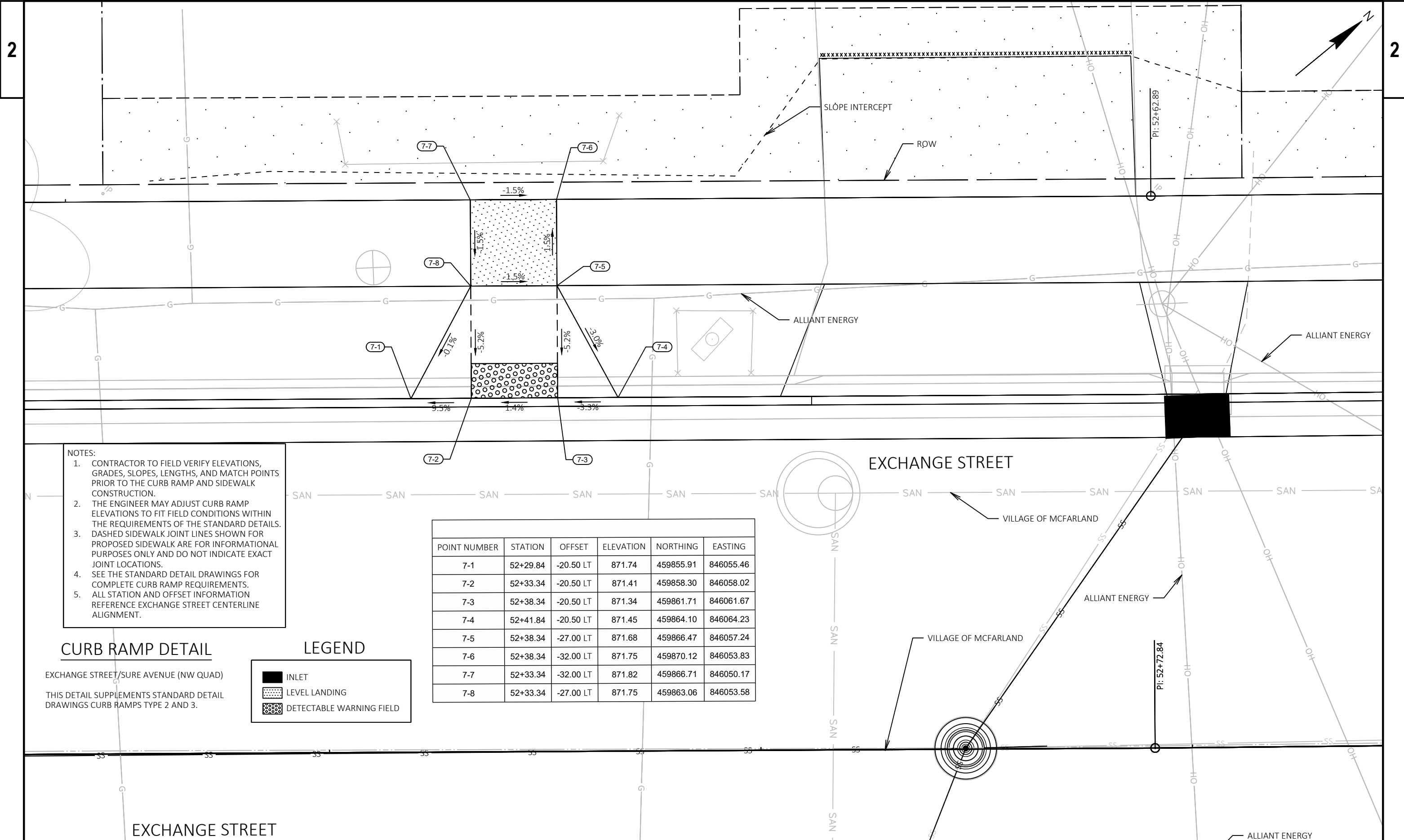
POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING	POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
6-29	45+29.16	35.50 RT	881.57	459337.04	845581.26	6-36	45+37.29	27.00 RT	880.88	459348.81	845581.41
6-30	45+37.29	32.00 RT	880.95	459345.15	845584.82	6-37	45+30.43	27.00 RT	881.16	459344.13	845576.39
6-31	45+42.29	32.00 RT	881.02	459348.56	845588.48	6-38	45+30.43	32.00 RT	881.23	459340.47	845579.80
6-32	45+42.29	27.00 RT	880.95	459352.22	845585.07	6-39	45+42.29	18.11 RT	880.87	459358.72	845579.00
6-33	45+45.79	14.67 RT	881.13	459363.62	845579.22	6-40	45+29.24	32.00 RT	881.22	459339.66	845578.94
6-34	45+42.29	15.52 RT	880.85	459360.61	845577.24						
6-35	45+37.29	18.10 RT	880.93	459355.31	845575.34						

CURB RAMP DETAIL

EXCHANGE STREET/RENEE COURT (SE QUAD)
 THIS DETAIL SUPPLEMENTS STANDARD DETAIL DRAWINGS CURB RAMPS TYPE 2 AND 3.

LEGEND

-  INLET
-  LEVEL LANDING
-  DETECTABLE WARNING FIELD



- NOTES:
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 2. THE ENGINEER MAY ADJUST CURB RAMP ELEVATIONS TO FIT FIELD CONDITIONS WITHIN THE REQUIREMENTS OF THE STANDARD DETAILS.
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 4. SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS.
 5. ALL STATION AND OFFSET INFORMATION REFERENCE EXCHANGE STREET CENTERLINE ALIGNMENT.

POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
7-1	52+29.84	-20.50 LT	871.74	459855.91	846055.46
7-2	52+33.34	-20.50 LT	871.41	459858.30	846058.02
7-3	52+38.34	-20.50 LT	871.34	459861.71	846061.67
7-4	52+41.84	-20.50 LT	871.45	459864.10	846064.23
7-5	52+38.34	-27.00 LT	871.68	459866.47	846057.24
7-6	52+38.34	-32.00 LT	871.75	459870.12	846053.83
7-7	52+33.34	-32.00 LT	871.82	459866.71	846050.17
7-8	52+33.34	-27.00 LT	871.75	459863.06	846053.58

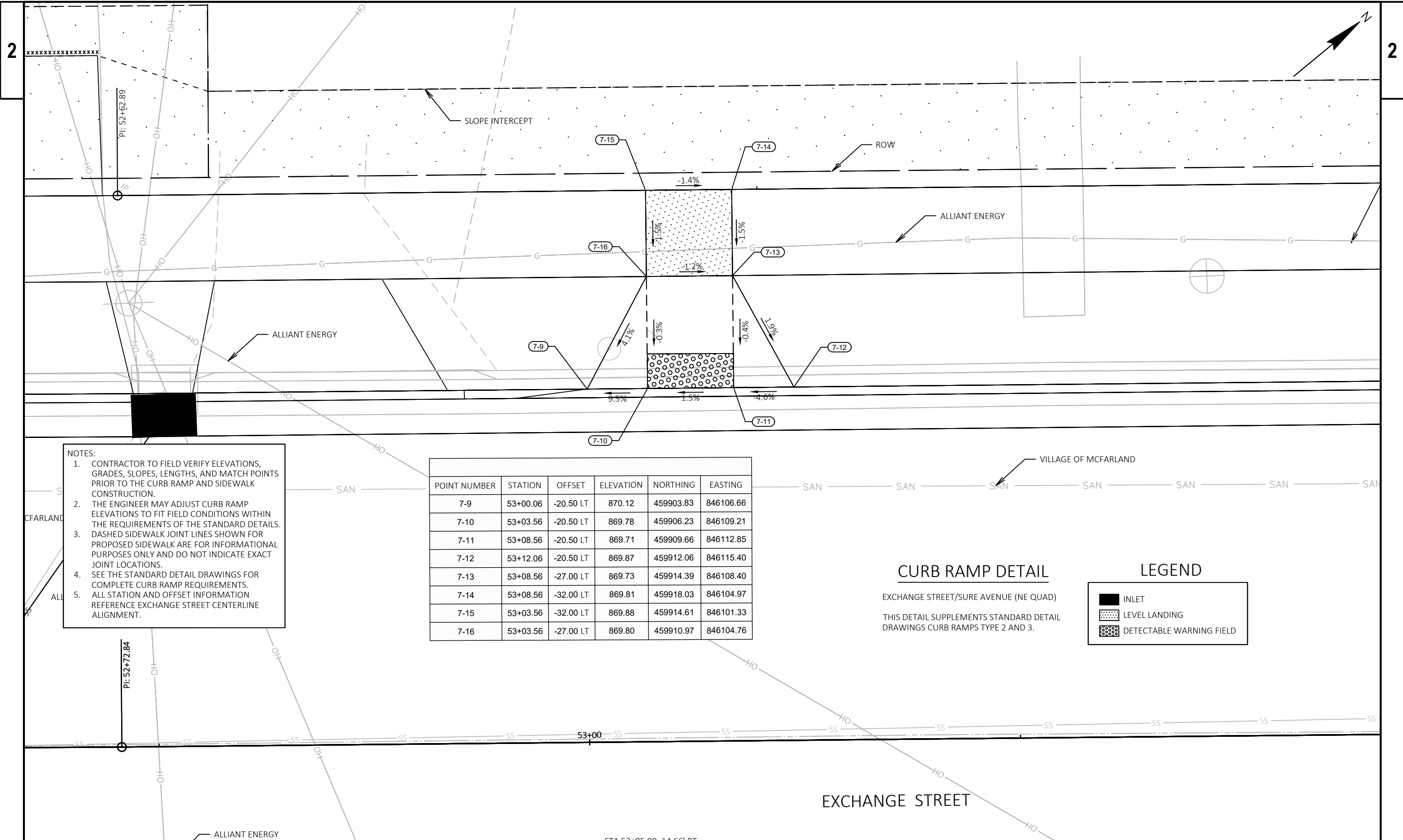
CURB RAMP DETAIL

LEGEND

EXCHANGE STREET/SURE AVENUE (NW QUAD)
 THIS DETAIL SUPPLEMENTS STANDARD DETAIL DRAWINGS CURB RAMPS TYPE 2 AND 3.

INLET
 LEVEL LANDING
 DETECTABLE WARNING FIELD

EXCHANGE STREET



- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATIONS, GRADES, SLOPES, LENGTHS, AND MATCH POINTS PRIOR TO THE CURB RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST CURB RAMP ELEVATIONS TO FIT FIELD CONDITIONS WITHIN THE REQUIREMENTS OF THE STANDARD DETAILS.
 3. DASHED SIDEWALK JOINT LINES SHOWN FOR PROPOSED SIDEWALK ARE FOR INFORMATIONAL PURPOSES ONLY AND DO NOT INDICATE EXACT JOINT LOCATIONS.
 4. SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS.
 5. ALL STATION AND OFFSET INFORMATION REFERENCE EXCHANGE STREET CENTERLINE ALIGNMENT.

POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
7-9	53+00.06	-20.50 LT	870.12	459903.83	846106.66
7-10	53+03.56	-20.50 LT	869.78	459906.23	846109.21
7-11	53+08.56	-20.50 LT	869.71	459909.66	846112.85
7-12	53+12.06	-20.50 LT	869.87	459912.06	846115.40
7-13	53+08.56	-27.00 LT	869.73	459914.39	846108.40
7-14	53+08.56	-32.00 LT	869.81	459918.03	846104.97
7-15	53+03.56	-32.00 LT	869.88	459914.61	846101.33
7-16	53+03.56	-27.00 LT	869.80	459910.97	846104.76

CURB RAMP DETAIL

EXCHANGE STREET/SURE AVENUE (NE QUAD)
 THIS DETAIL SUPPLEMENTS STANDARD DETAIL DRAWINGS CURB RAMPS TYPE 2 AND 3.

LEGEND

- INLET
- LEVEL LANDING
- DETECTABLE WARNING FIELD

POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING	POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
7-17	52+38.34	31.98 RT	873.40	459823.33	846097.47	7-25	52+33.34	31.99 RT	873.48	459819.92	846093.82
7-18	52+46.76	35.47 RT	873.35	459826.53	846106.01	7-26	52+38.34	26.98 RT	873.33	459826.99	846094.06
7-19	52+46.68	31.97 RT	873.08	459829.03	846103.56	7-27	52+45.47	31.97 RT	873.10	459828.20	846102.68
7-20	52+45.47	26.97 RT	873.09	459831.86	846099.27	7-28	52+33.34	17.94 RT	873.11	459830.19	846084.23
7-21	52+38.34	17.94 RT	873.10	459833.60	846087.89						
7-22	52+33.34	15.46 RT	873.07	459832.00	846082.55						
7-23	52+29.84	14.67 RT	873.38	459830.20	846079.44						
7-24	52+33.34	26.97 RT	873.40	459823.59	846090.39						

EXCHANGE STREET

SURE AVENUE

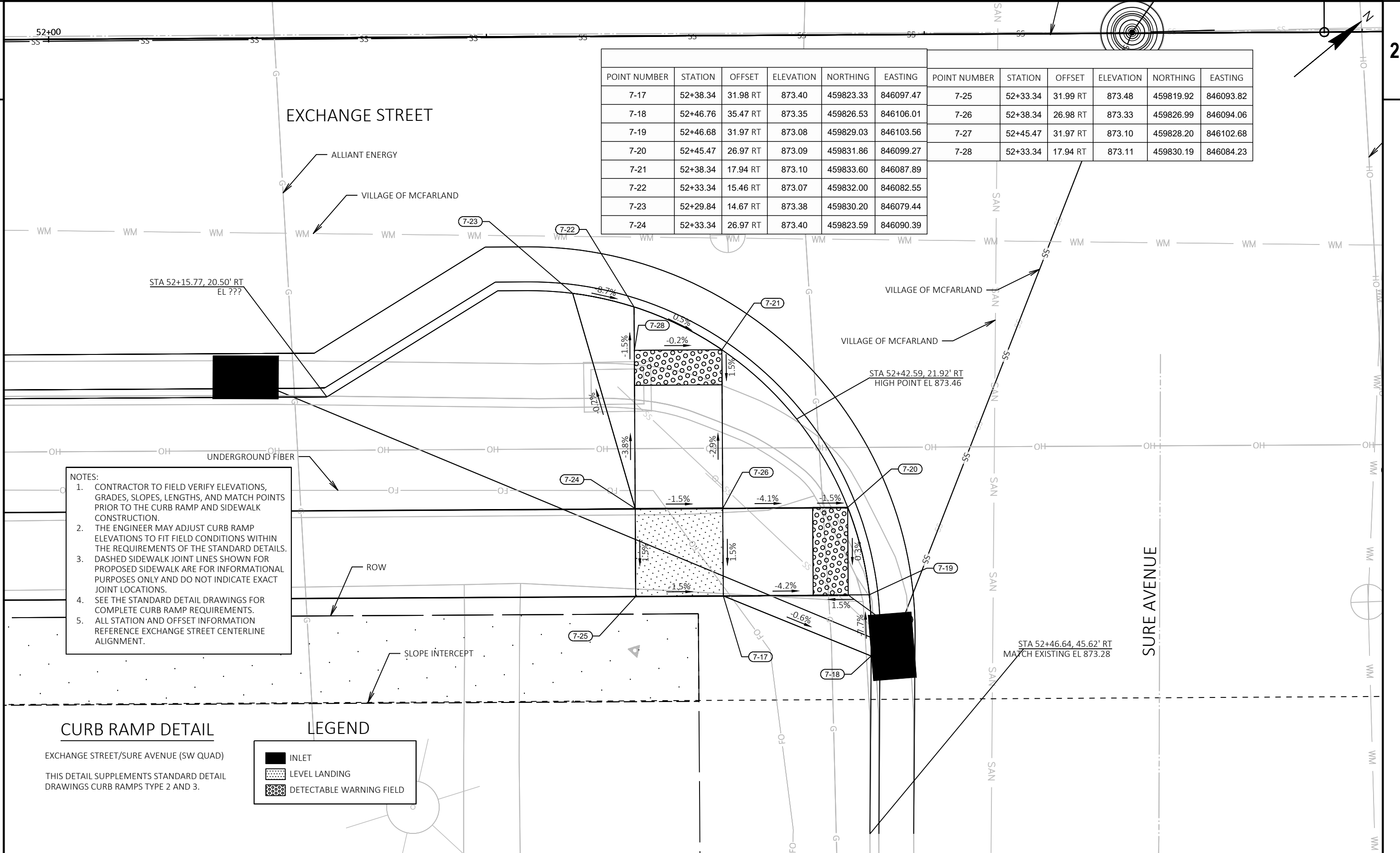
- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATIONS, GRADES, SLOPES, LENGTHS, AND MATCH POINTS PRIOR TO THE CURB RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST CURB RAMP ELEVATIONS TO FIT FIELD CONDITIONS WITHIN THE REQUIREMENTS OF THE STANDARD DETAILS.
 3. DASHED SIDEWALK JOINT LINES SHOWN FOR PROPOSED SIDEWALK ARE FOR INFORMATIONAL PURPOSES ONLY AND DO NOT INDICATE EXACT JOINT LOCATIONS.
 4. SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS.
 5. ALL STATION AND OFFSET INFORMATION REFERENCE EXCHANGE STREET CENTERLINE ALIGNMENT.

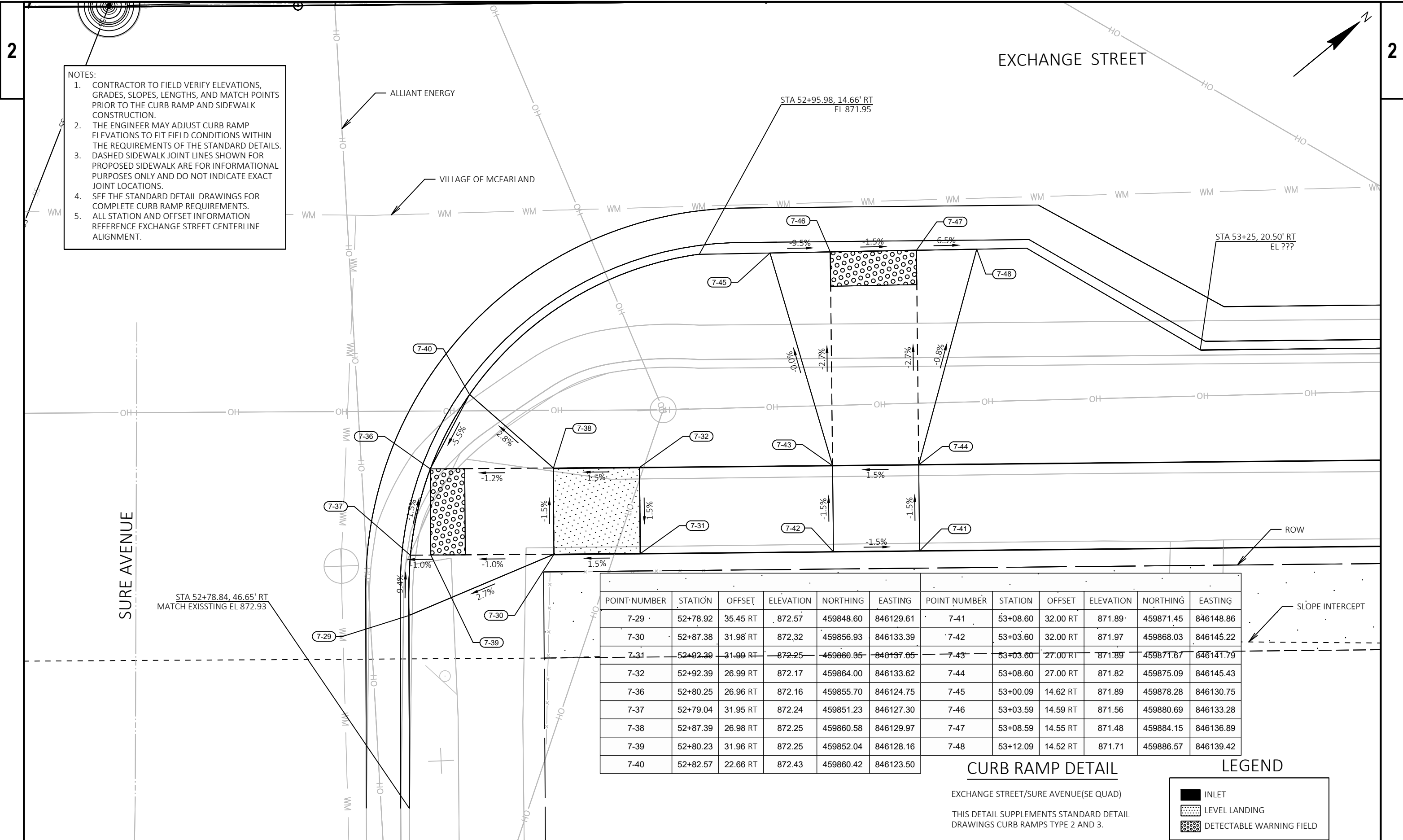
CURB RAMP DETAIL

EXCHANGE STREET/SURE AVENUE (SW QUAD)
 THIS DETAIL SUPPLEMENTS STANDARD DETAIL DRAWINGS CURB RAMPS TYPE 2 AND 3.

LEGEND

	INLET
	LEVEL LANDING
	DETECTABLE WARNING FIELD





- NOTES:
1. CONTRACTOR TO FIELD VERIFY ELEVATIONS, GRADES, SLOPES, LENGTHS, AND MATCH POINTS PRIOR TO THE CURB RAMP AND SIDEWALK CONSTRUCTION.
 2. THE ENGINEER MAY ADJUST CURB RAMP ELEVATIONS TO FIT FIELD CONDITIONS WITHIN THE REQUIREMENTS OF THE STANDARD DETAILS.
 3. DASHED SIDEWALK JOINT LINES SHOWN FOR PROPOSED SIDEWALK ARE FOR INFORMATIONAL PURPOSES ONLY AND DO NOT INDICATE EXACT JOINT LOCATIONS.
 4. SEE THE STANDARD DETAIL DRAWINGS FOR COMPLETE CURB RAMP REQUIREMENTS.
 5. ALL STATION AND OFFSET INFORMATION REFERENCE EXCHANGE STREET CENTERLINE ALIGNMENT.

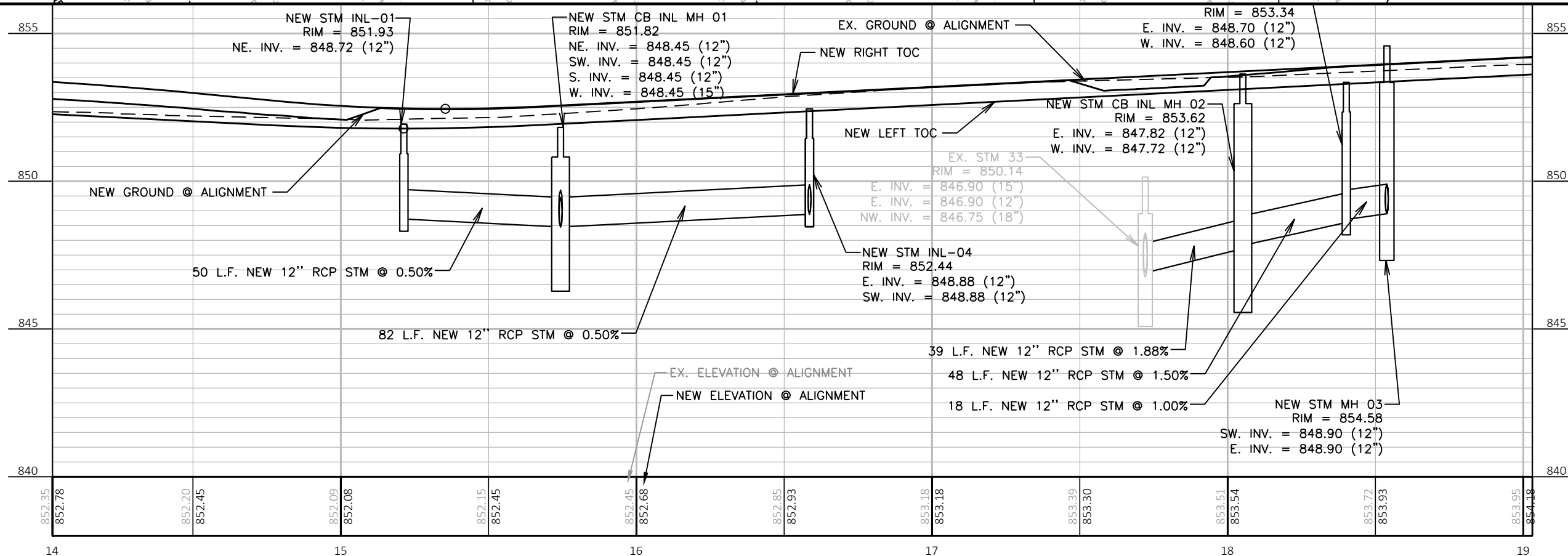
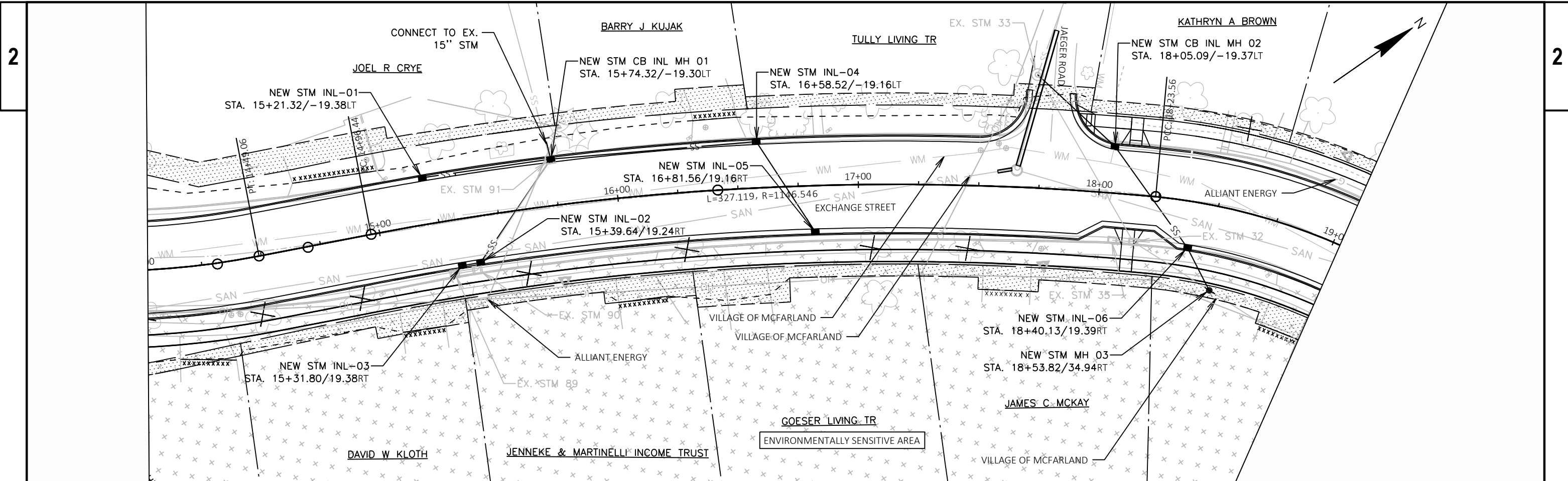
POINT-NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING	POINT NUMBER	STATION	OFFSET	ELEVATION	NORTHING	EASTING
7-29	52+78.92	35.45 RT	872.57	459848.60	846129.61	7-41	53+08.60	32.00 RT	871.89	459871.45	846148.86
7-30	52+87.38	31.98 RT	872.32	459856.93	846133.39	7-42	53+03.60	32.00 RT	871.97	459868.03	846145.22
7-31	52+92.39	31.99 RT	872.25	459860.35	846137.05	7-43	53+03.60	27.00 RT	871.89	459871.67	846141.79
7-32	52+92.39	26.99 RT	872.17	459864.00	846133.62	7-44	53+08.60	27.00 RT	871.82	459875.09	846145.43
7-36	52+80.25	26.96 RT	872.16	459855.70	846124.75	7-45	53+00.09	14.62 RT	871.89	459878.28	846130.75
7-37	52+79.04	31.95 RT	872.24	459851.23	846127.30	7-46	53+03.59	14.59 RT	871.56	459880.69	846133.28
7-38	52+87.39	26.98 RT	872.25	459860.58	846129.97	7-47	53+08.59	14.55 RT	871.48	459884.15	846136.89
7-39	52+80.23	31.96 RT	872.25	459852.04	846128.16	7-48	53+12.09	14.52 RT	871.71	459886.57	846139.42
7-40	52+82.57	22.66 RT	872.43	459860.42	846123.50						

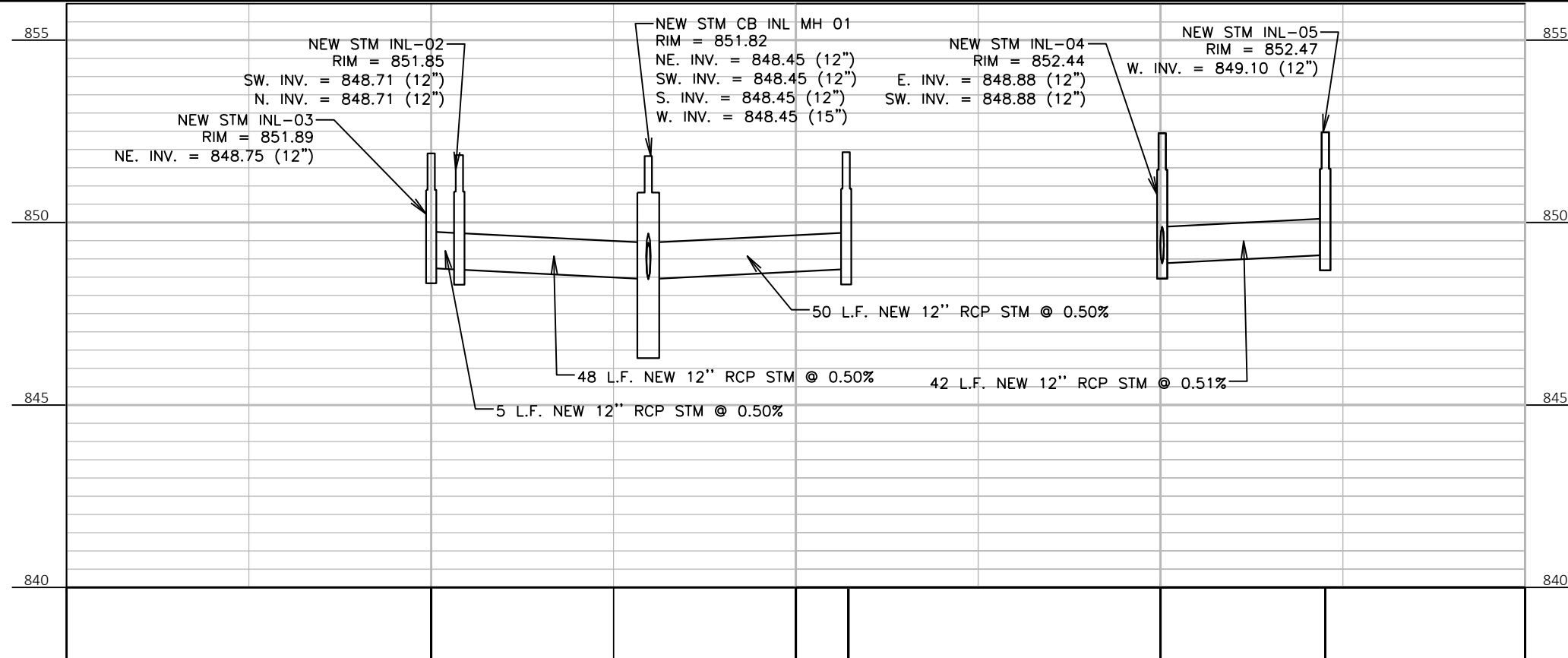
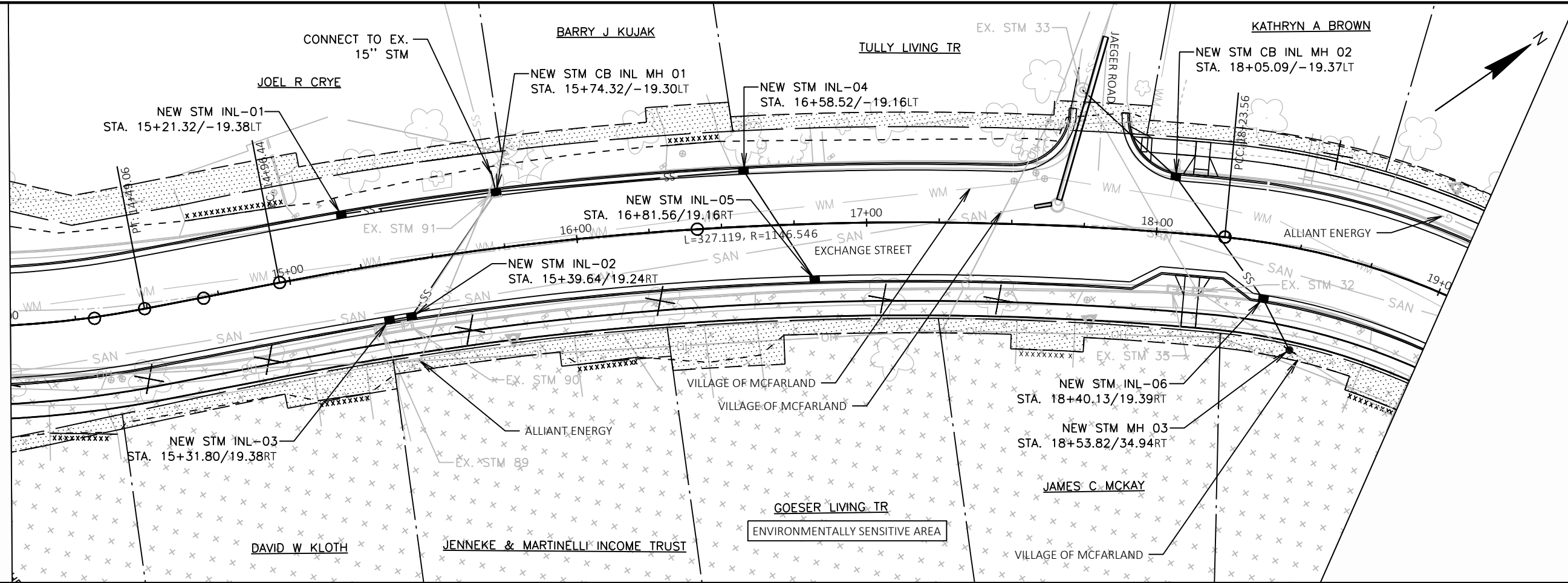
CURB RAMP DETAIL

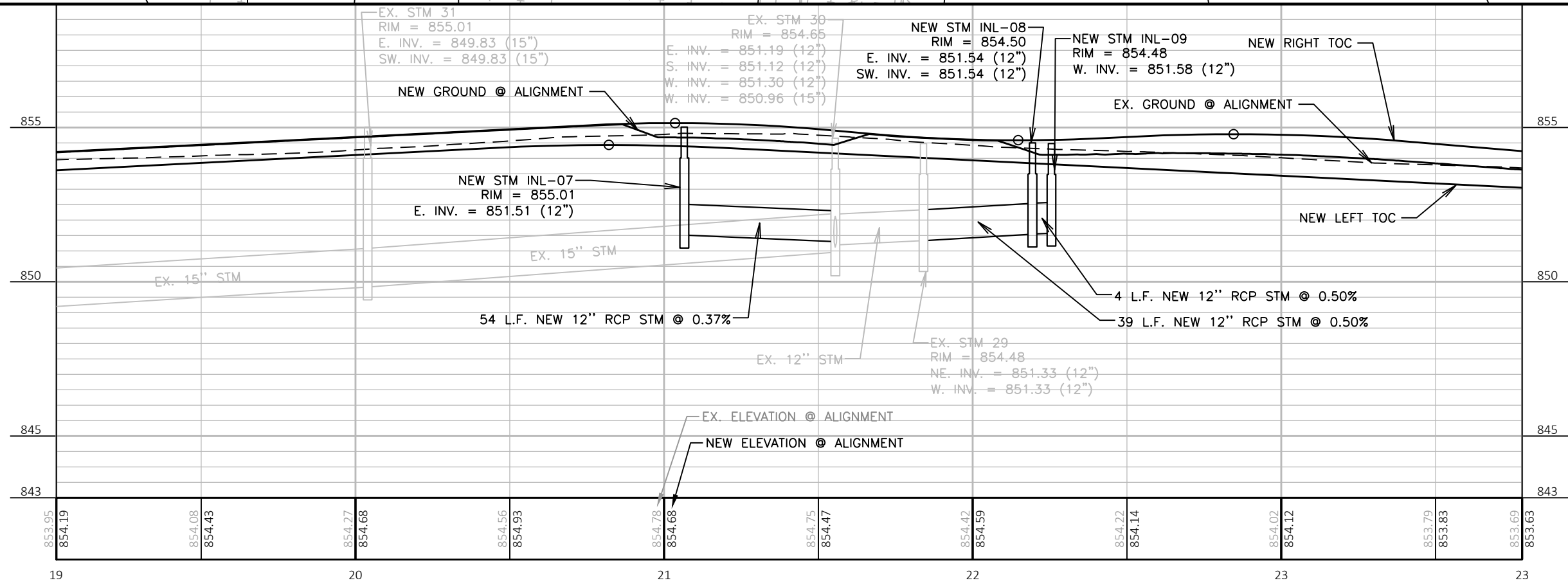
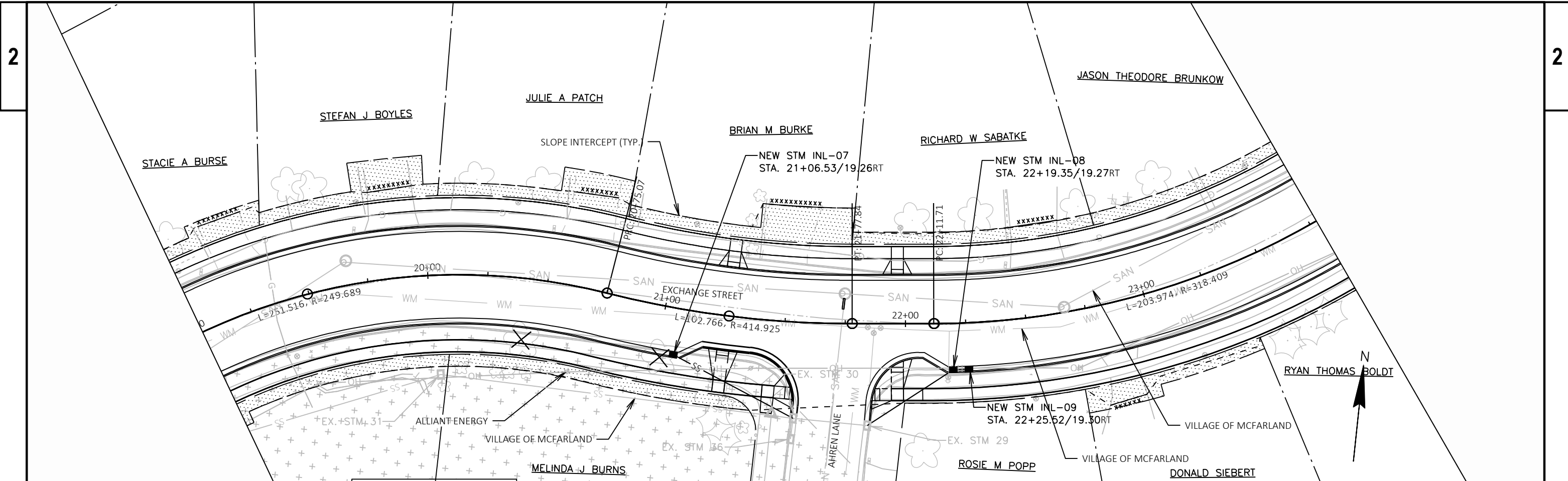
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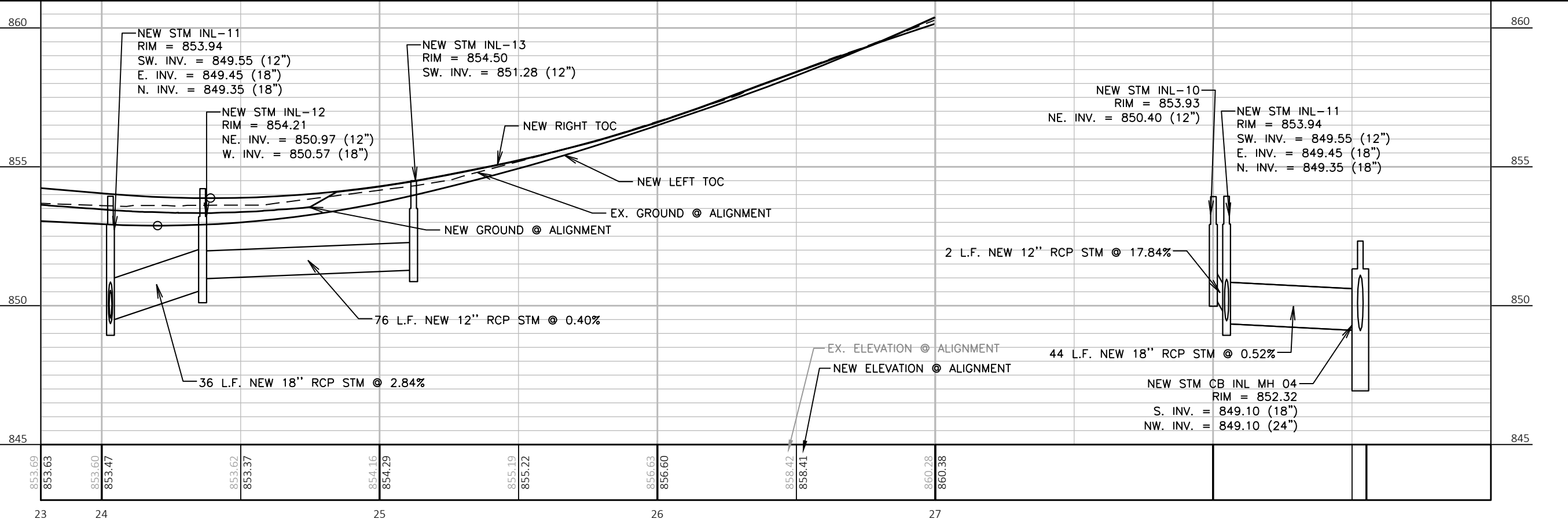
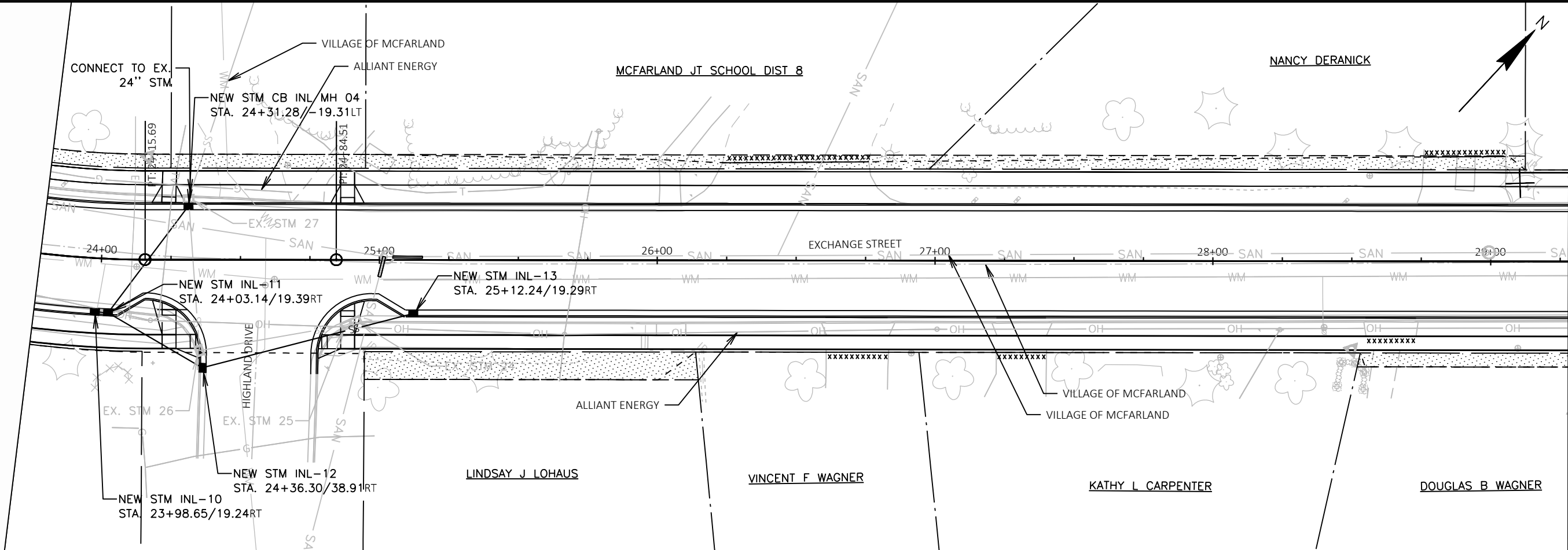
EXCHANGE STREET/SURE AVENUE(SE QUAD)
 THIS DETAIL SUPPLEMENTS STANDARD DETAIL DRAWINGS CURB RAMPS TYPE 2 AND 3.

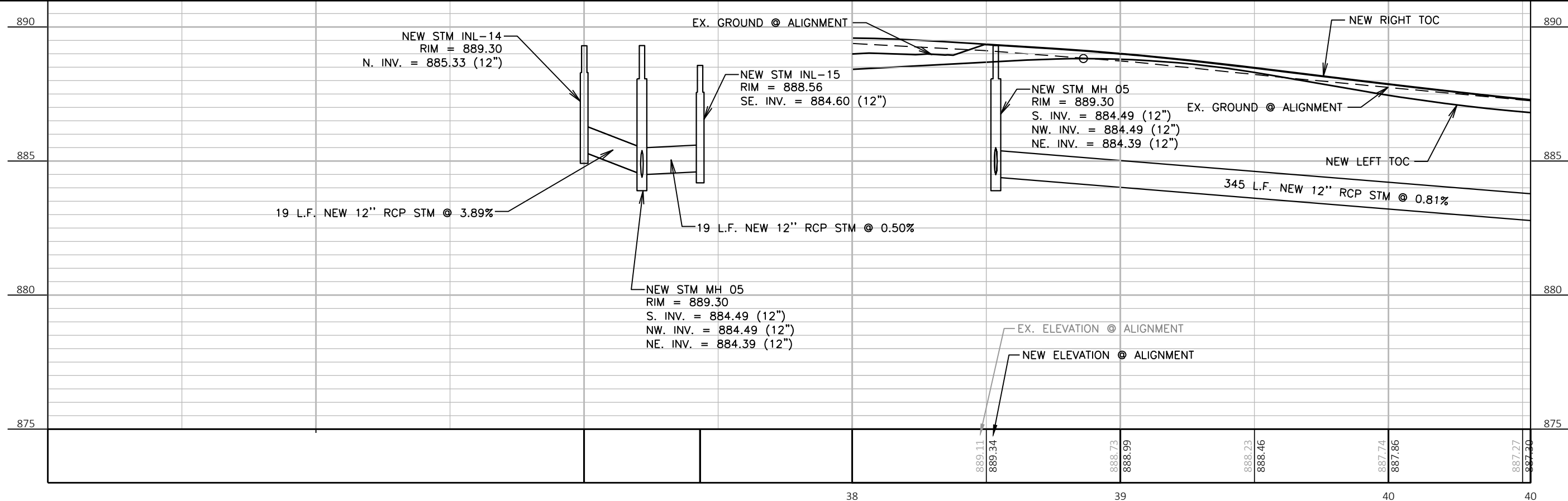
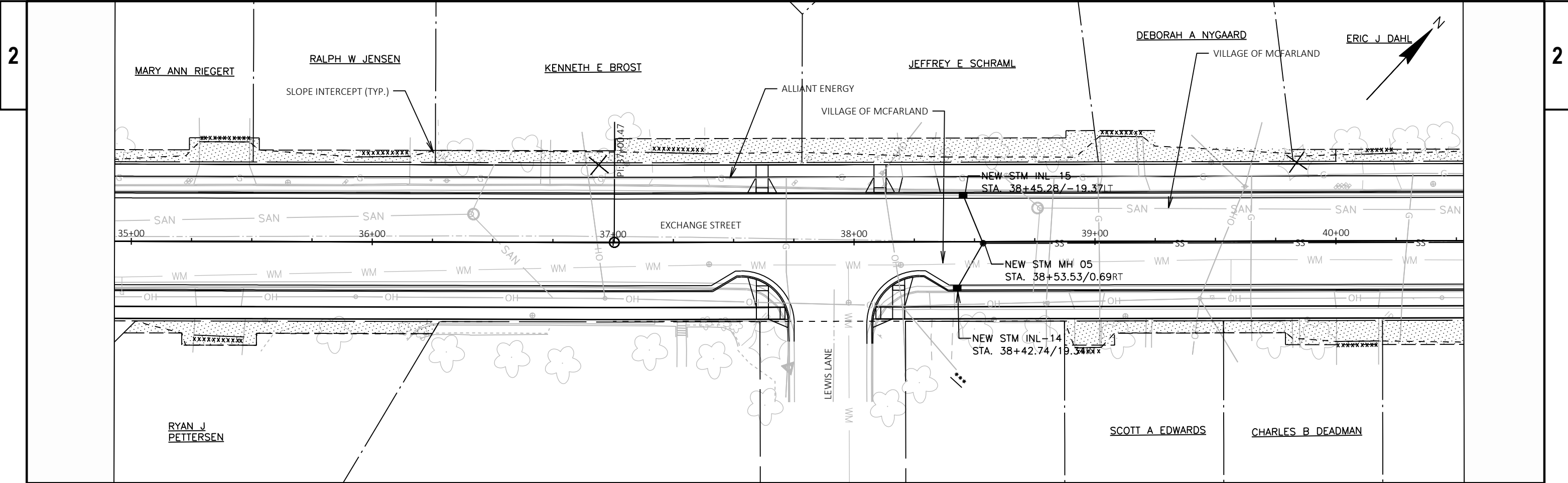
- INLET
- LEVEL LANDING
- DETECTABLE WARNING FIELD

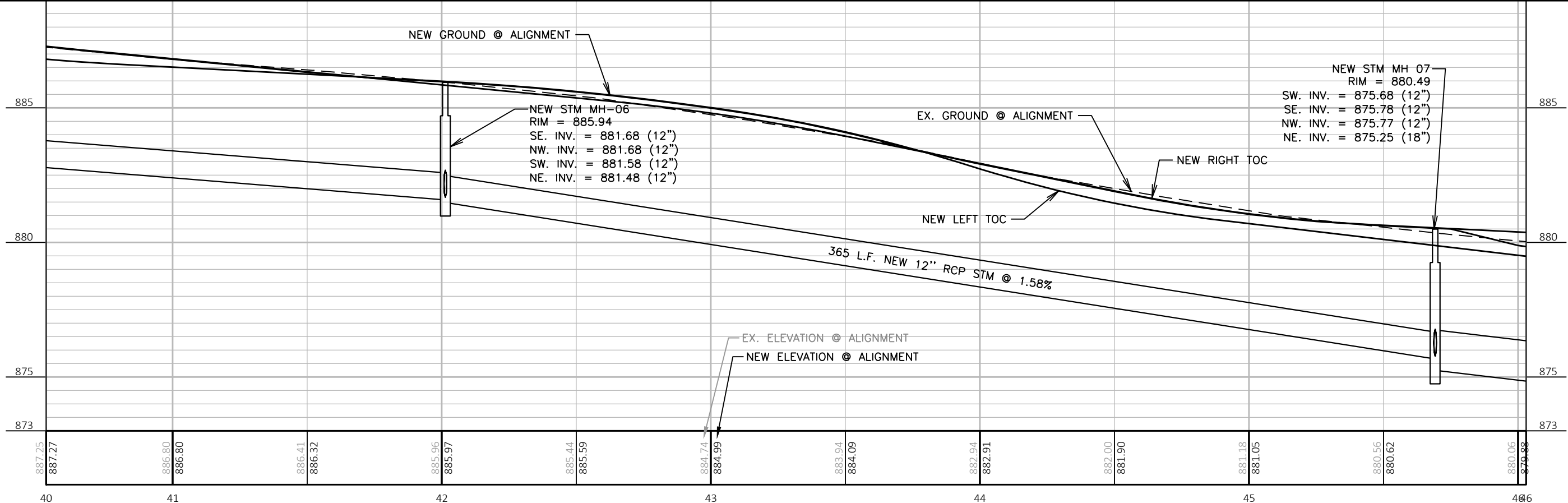
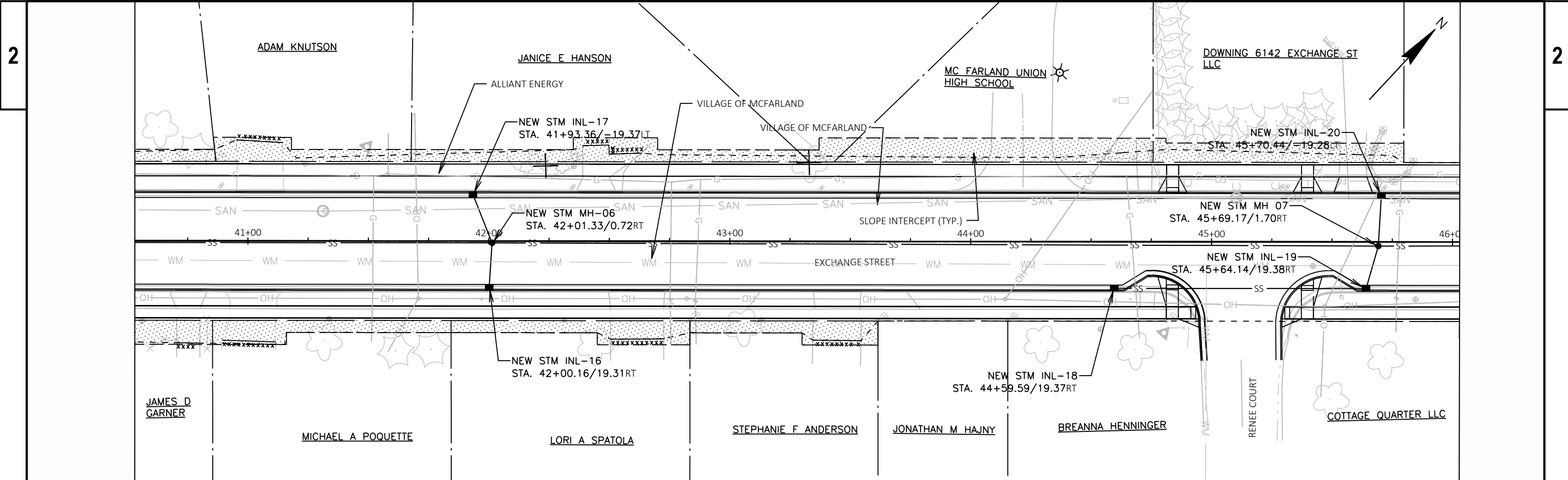


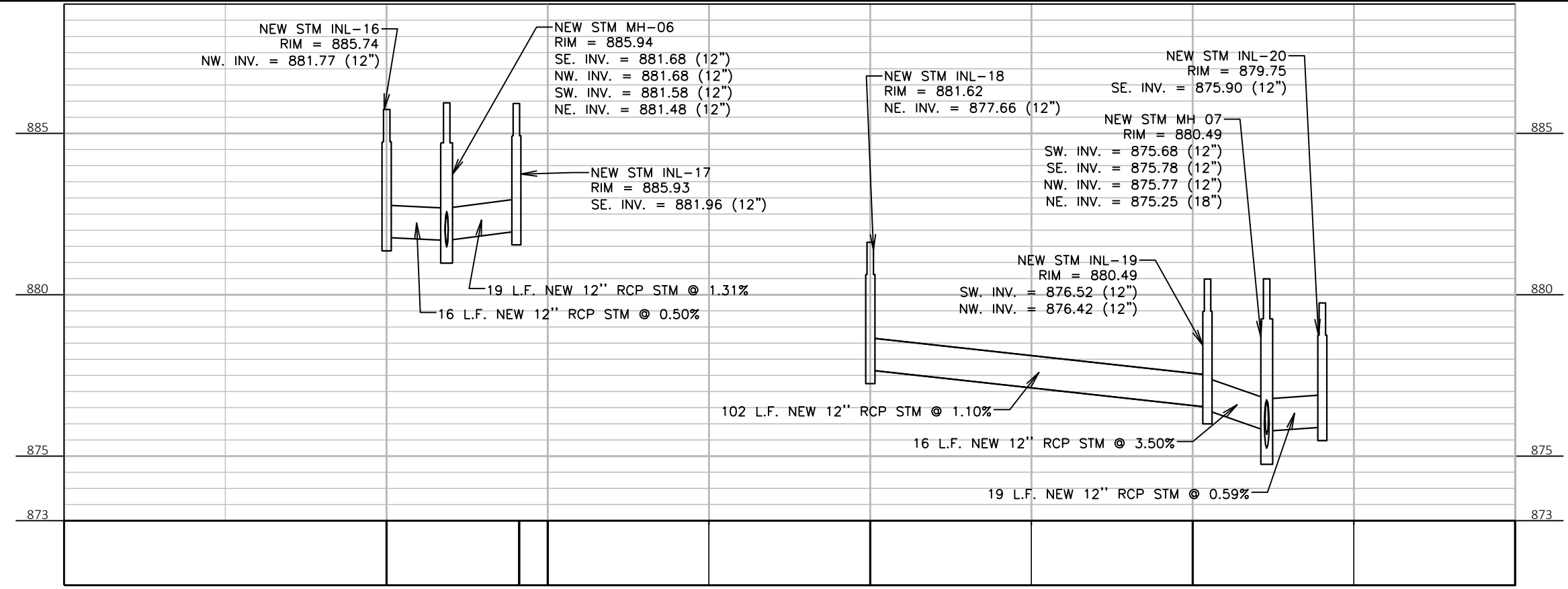
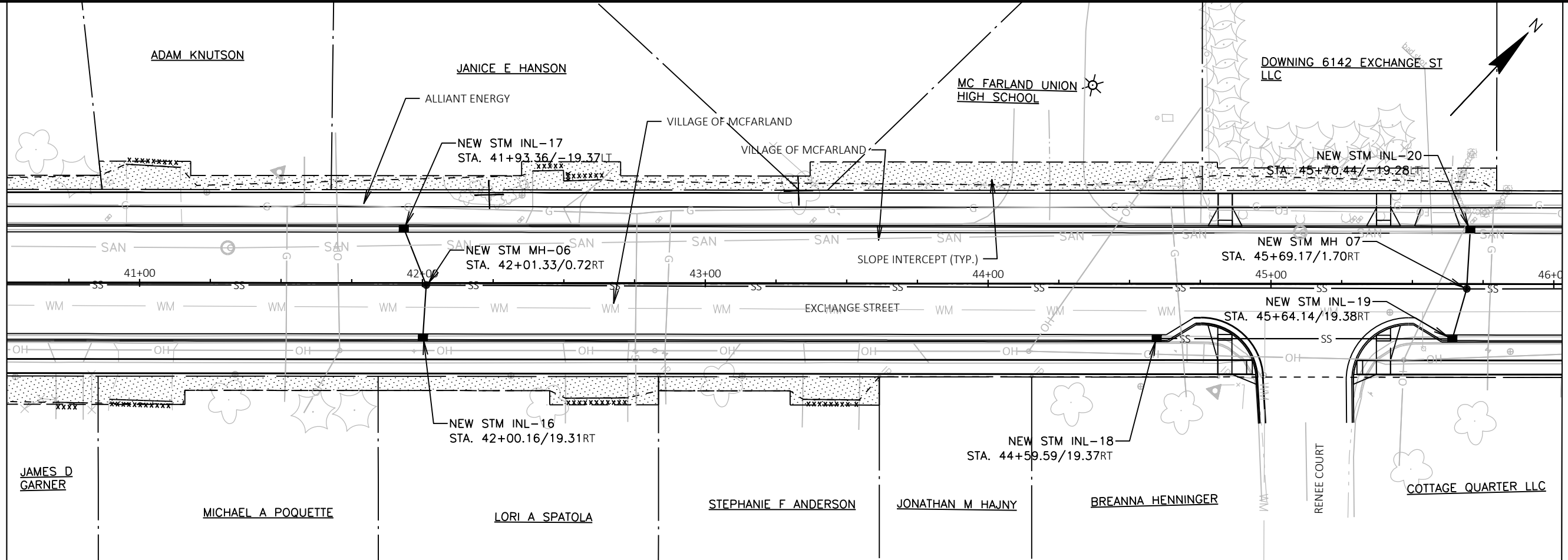


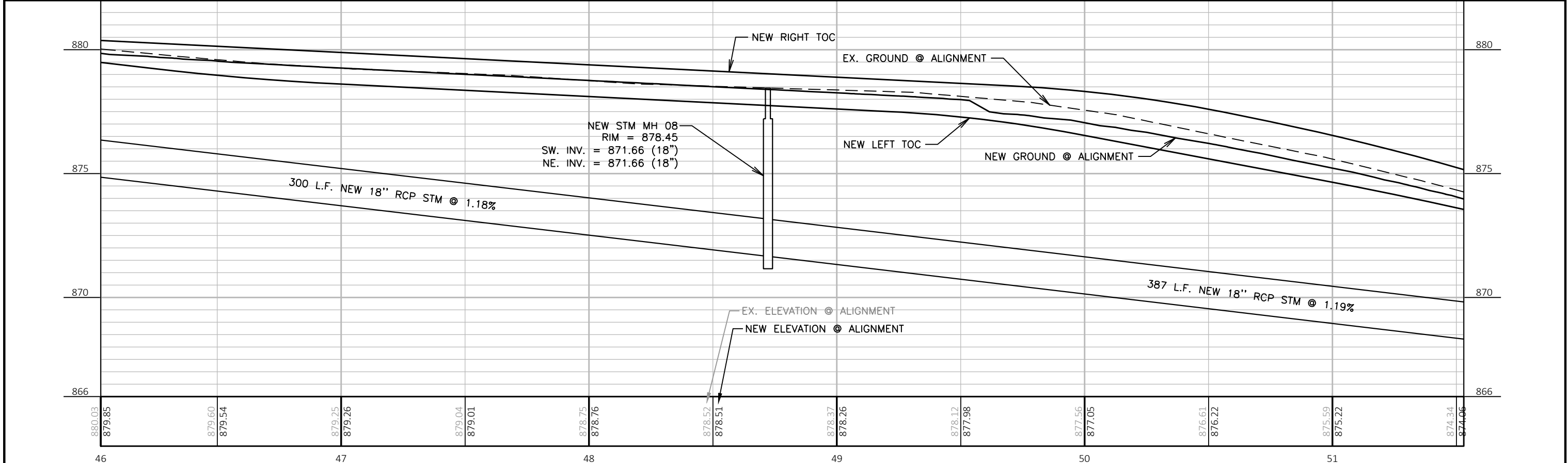
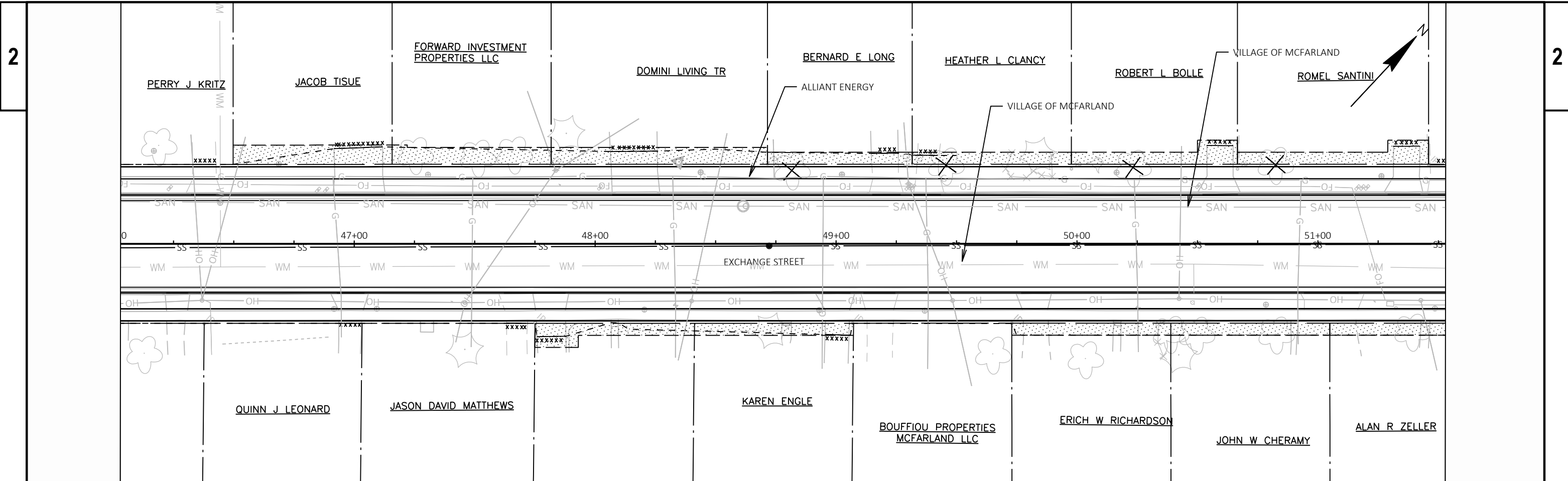




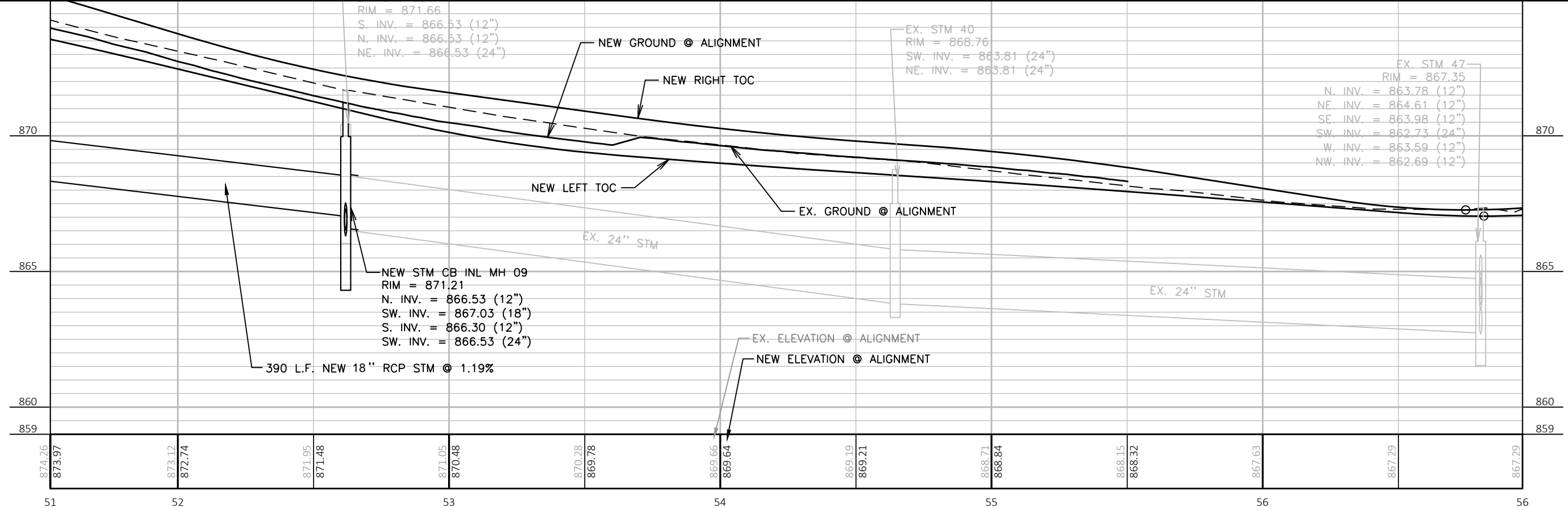
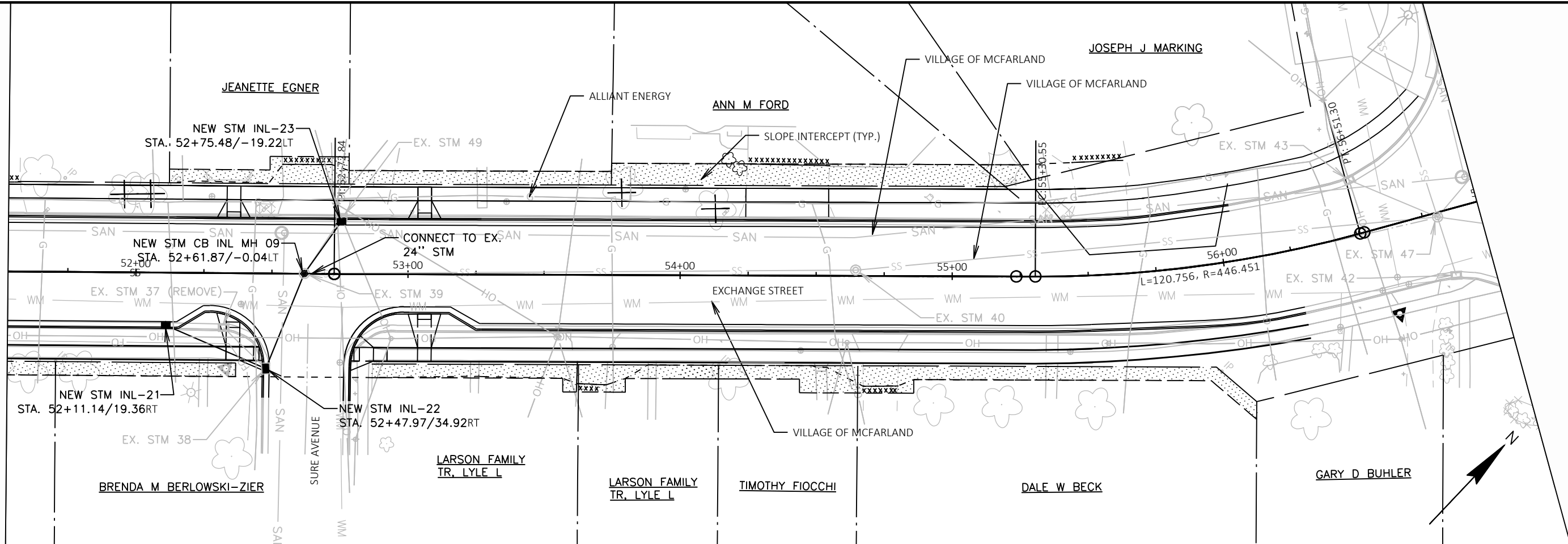


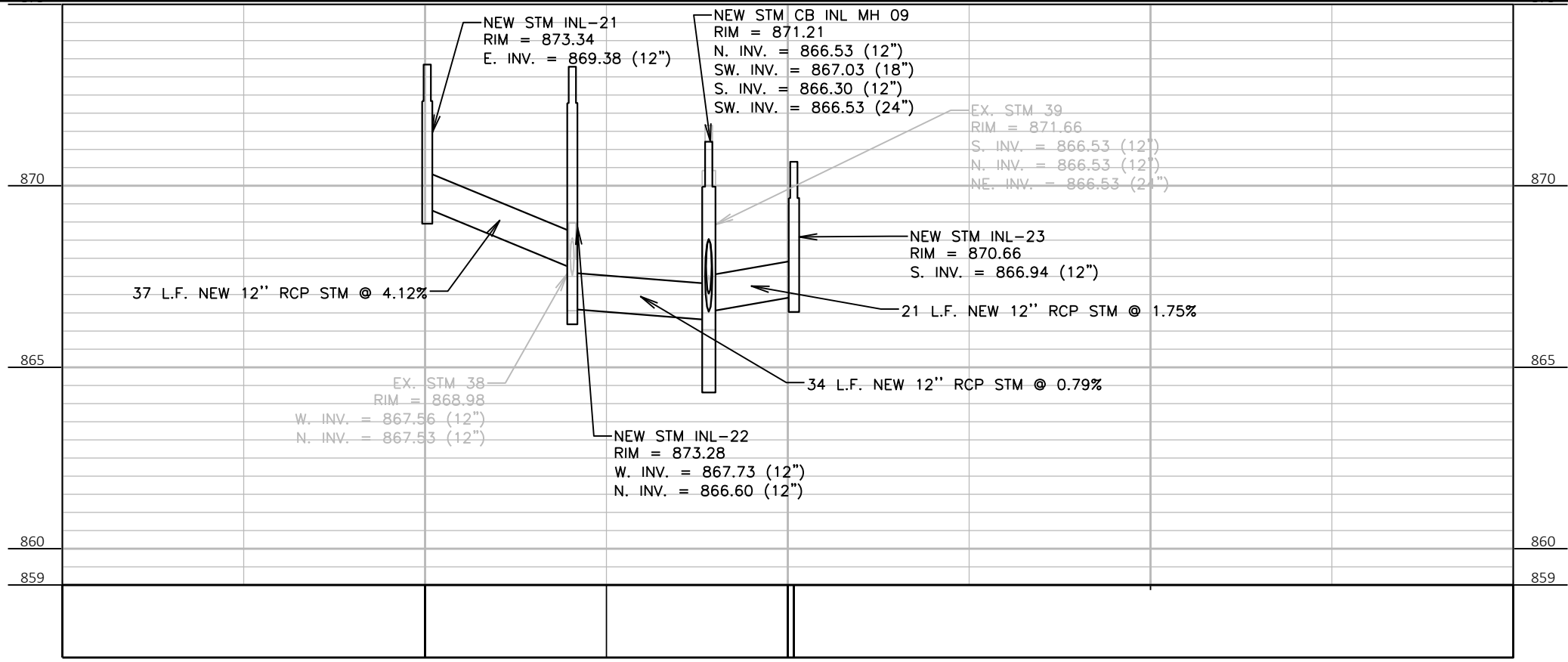
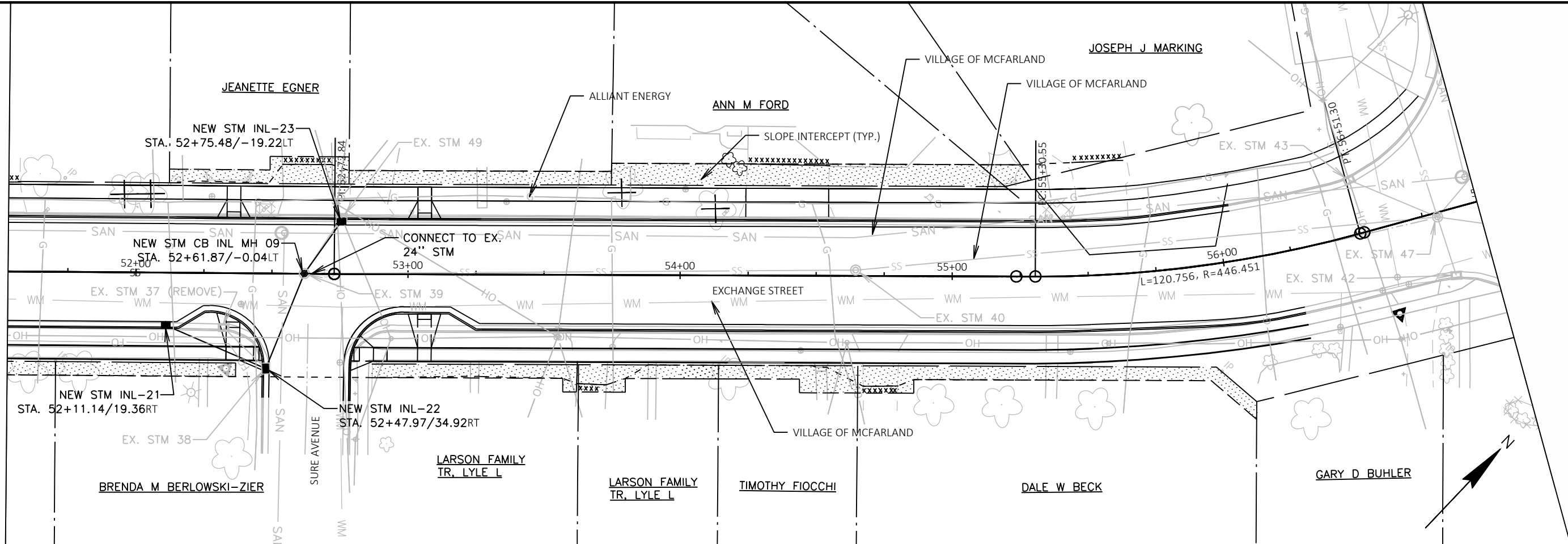


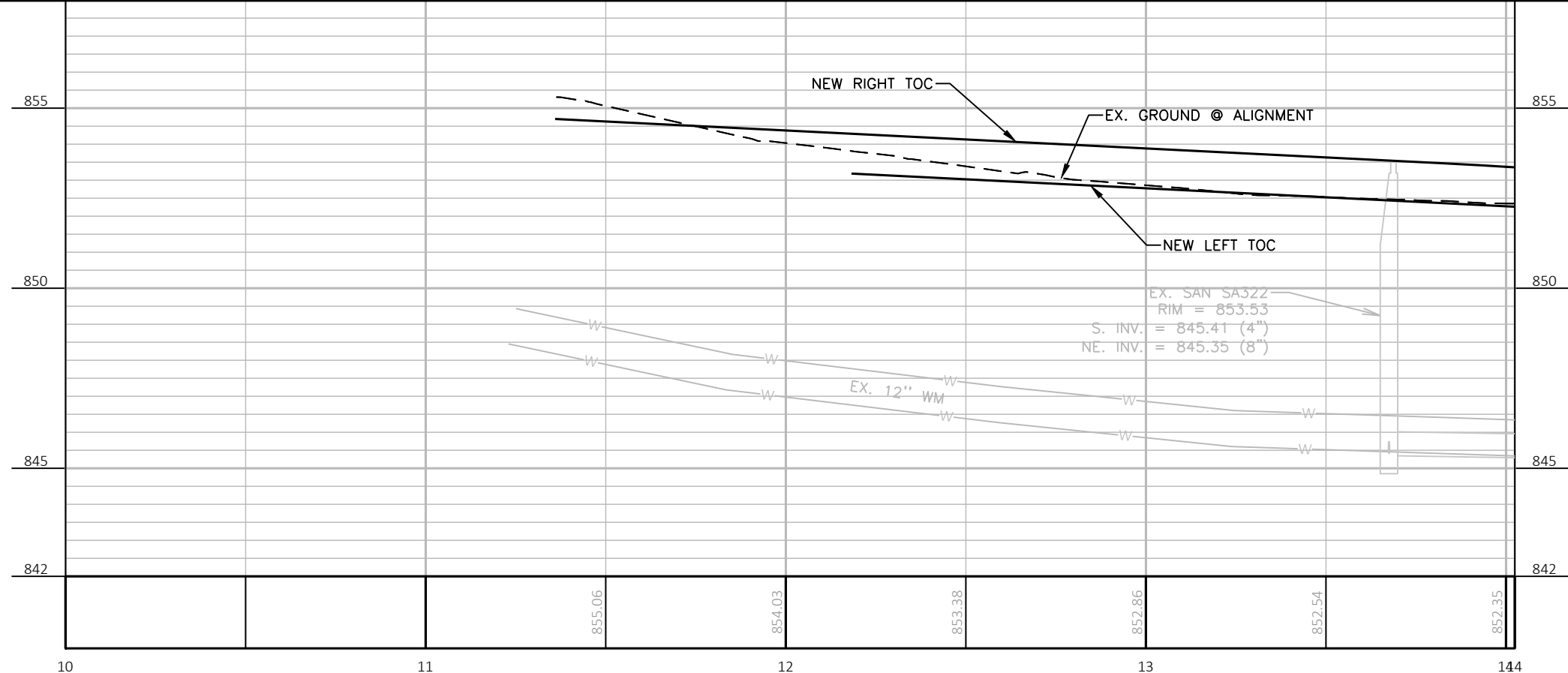
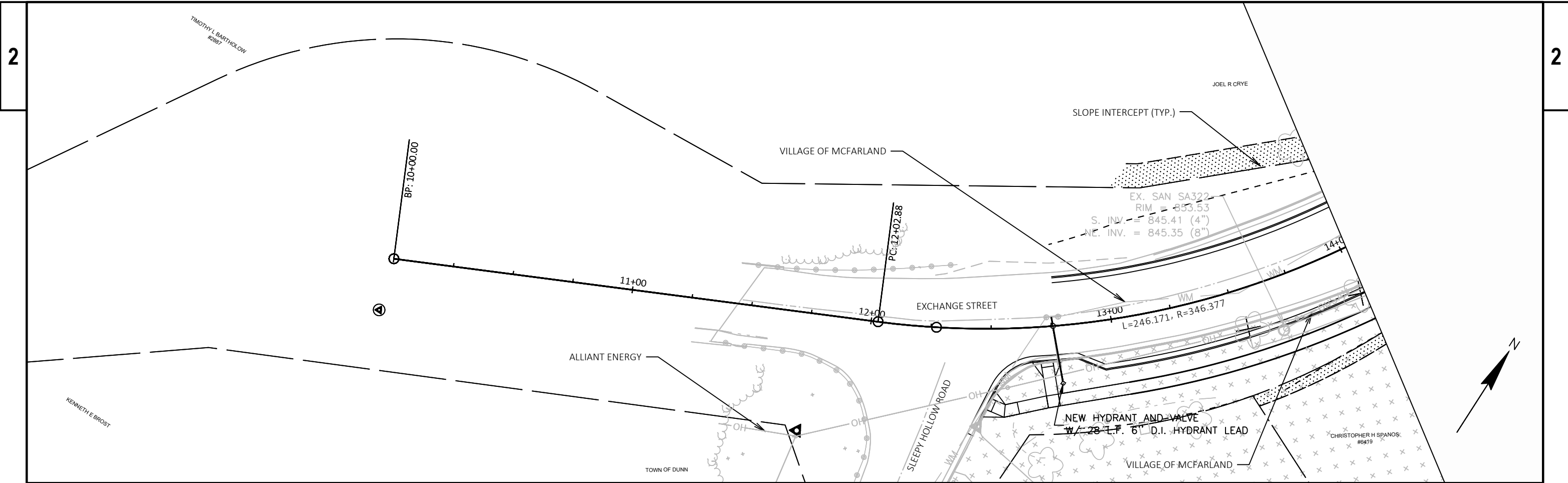


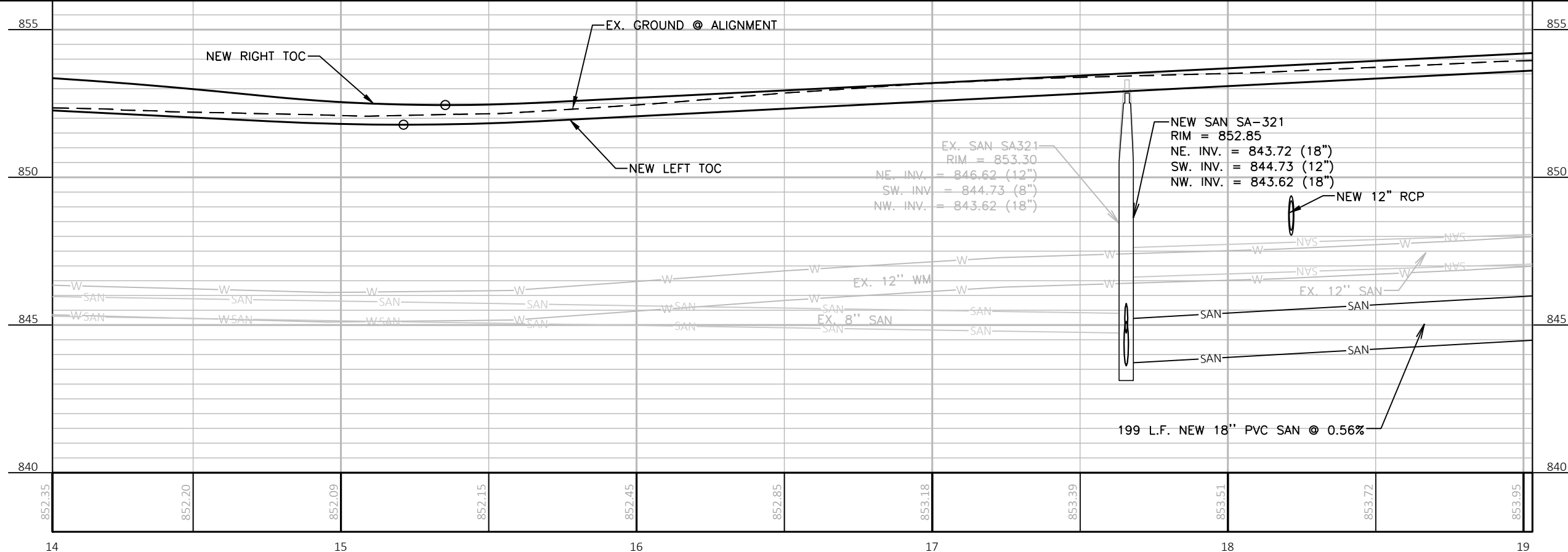
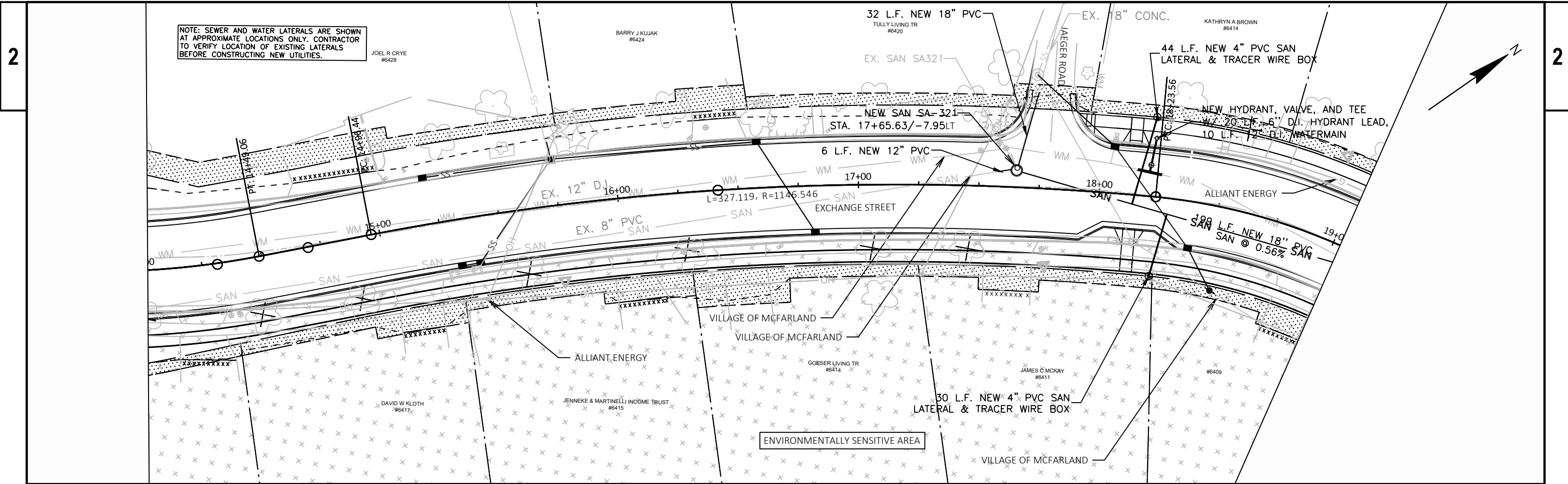


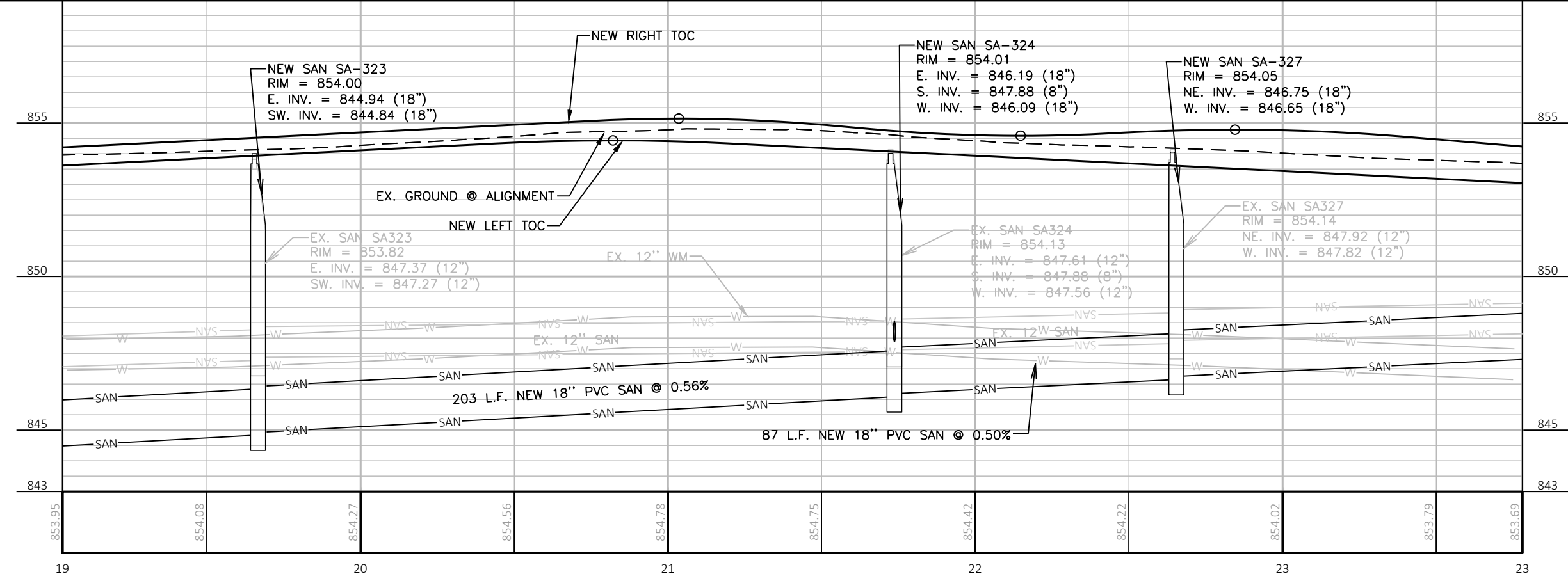
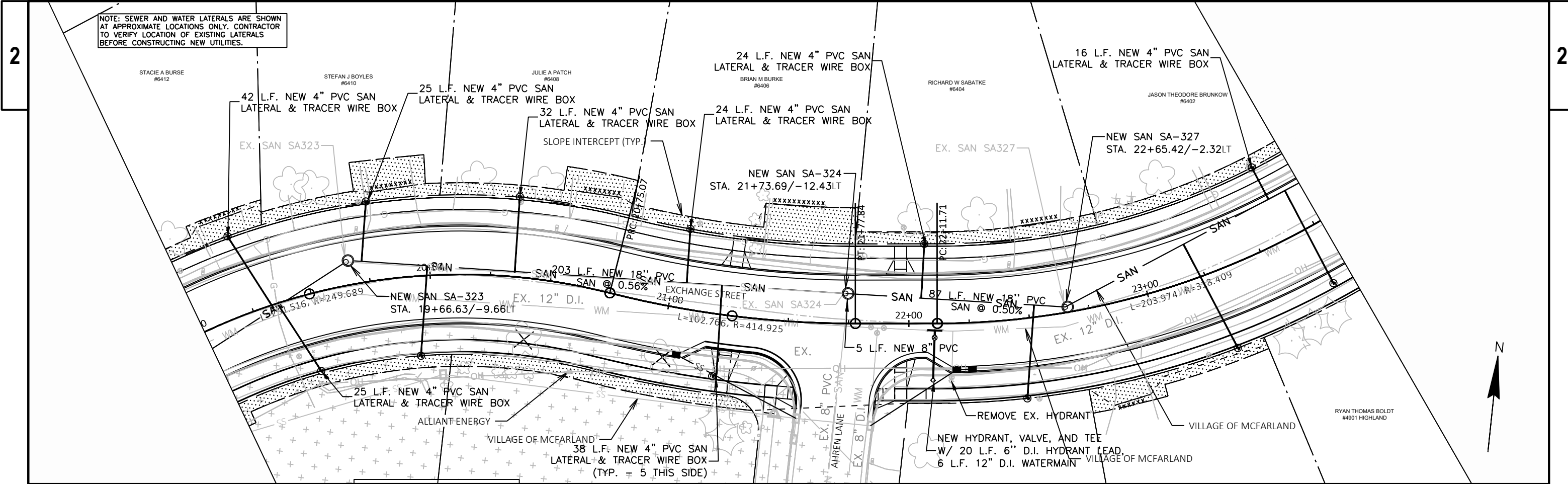
PROJECT NO: 5685-00-05 HWY: EXCHANGE STREET COUNTY: DANE PLAN AND PROFILE: STORM SEWER PLAN SHEET 42 E

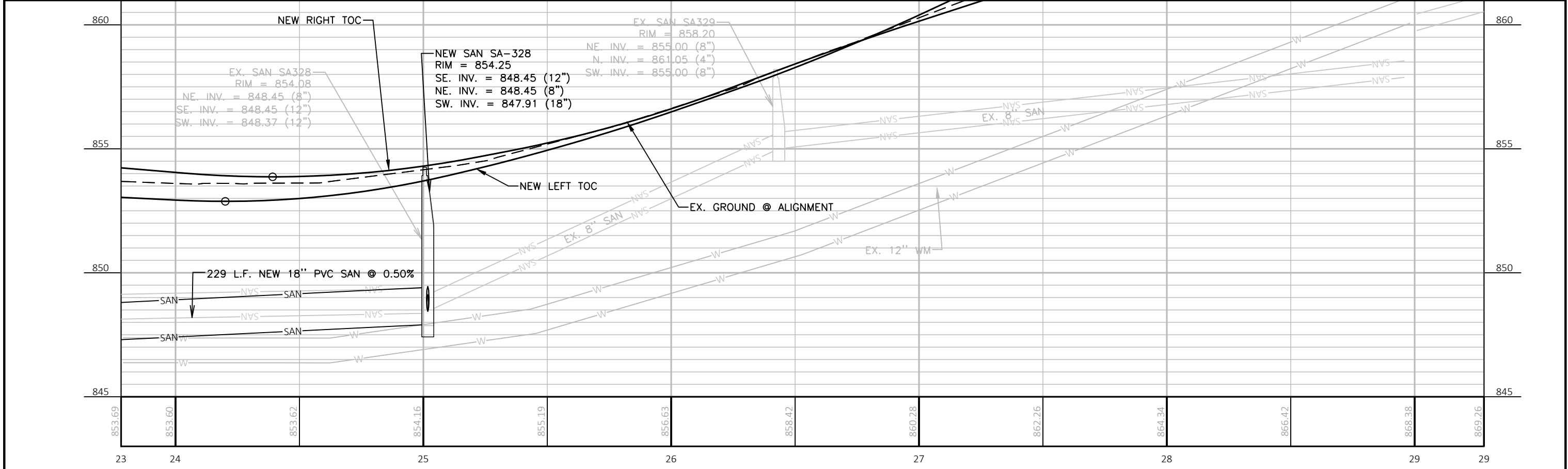
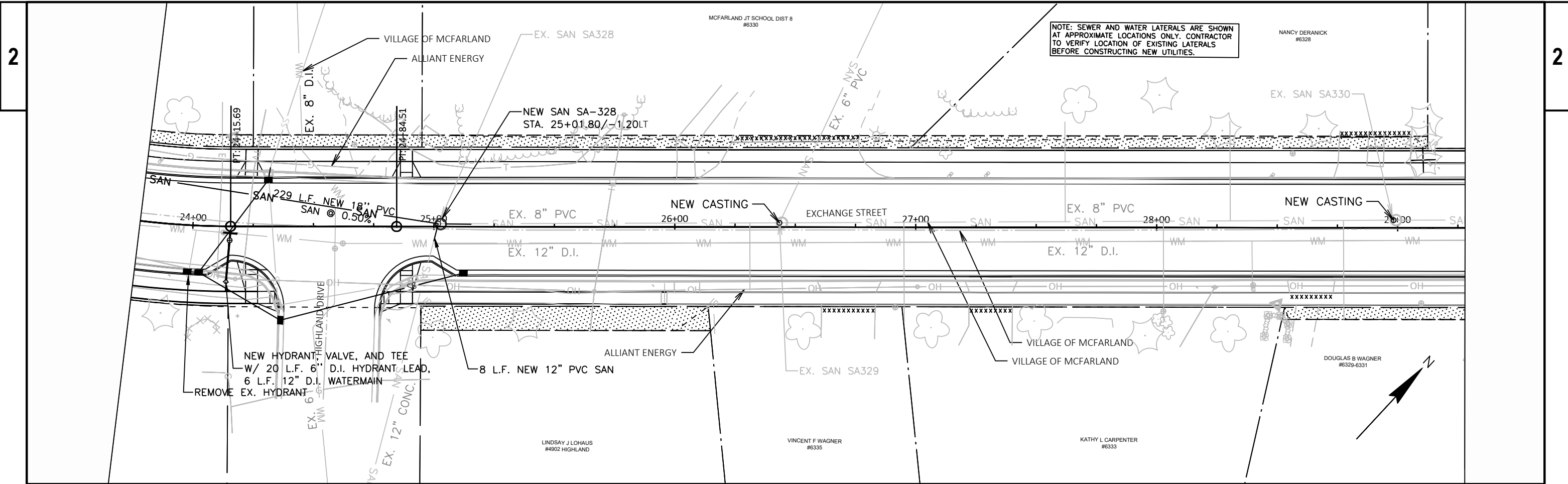




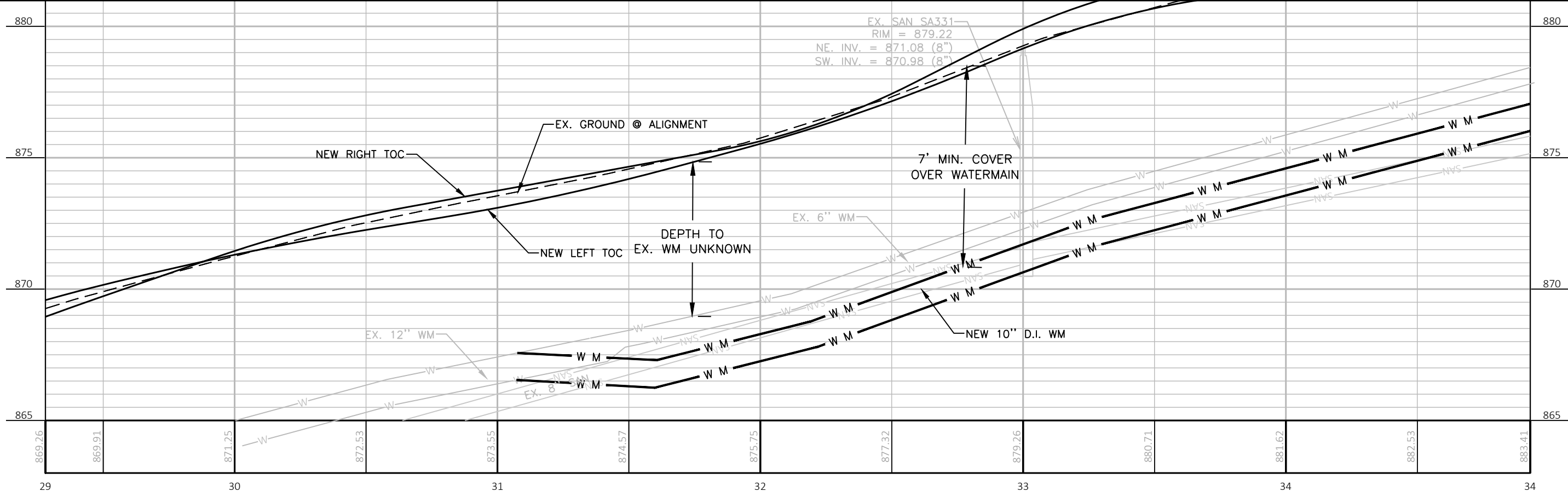
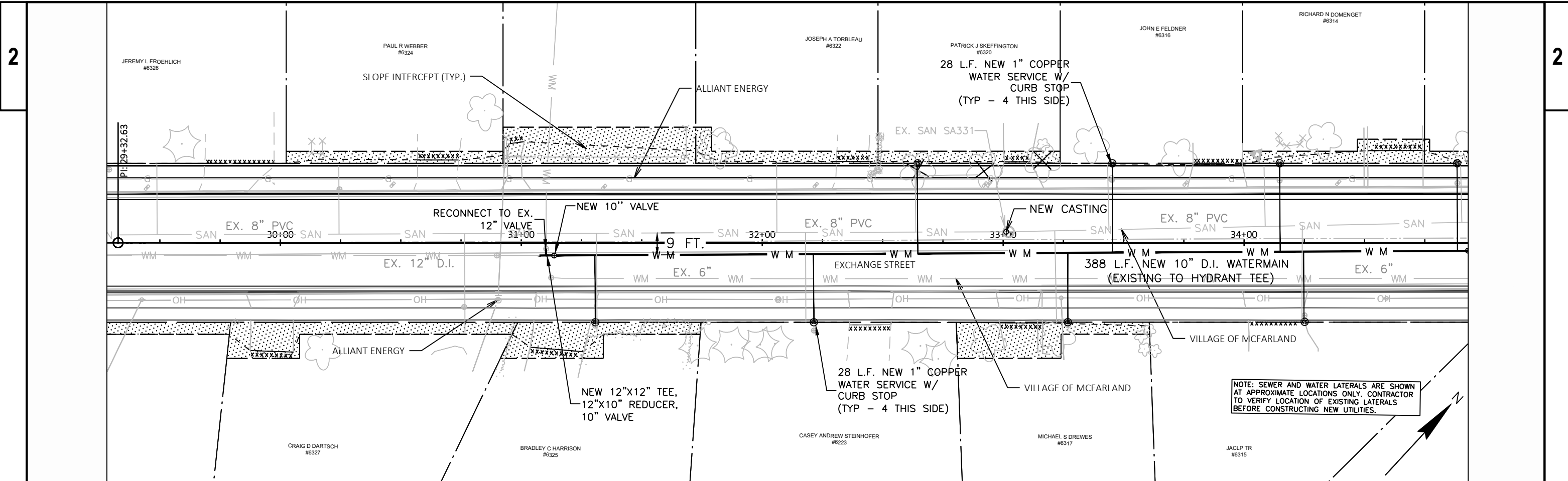


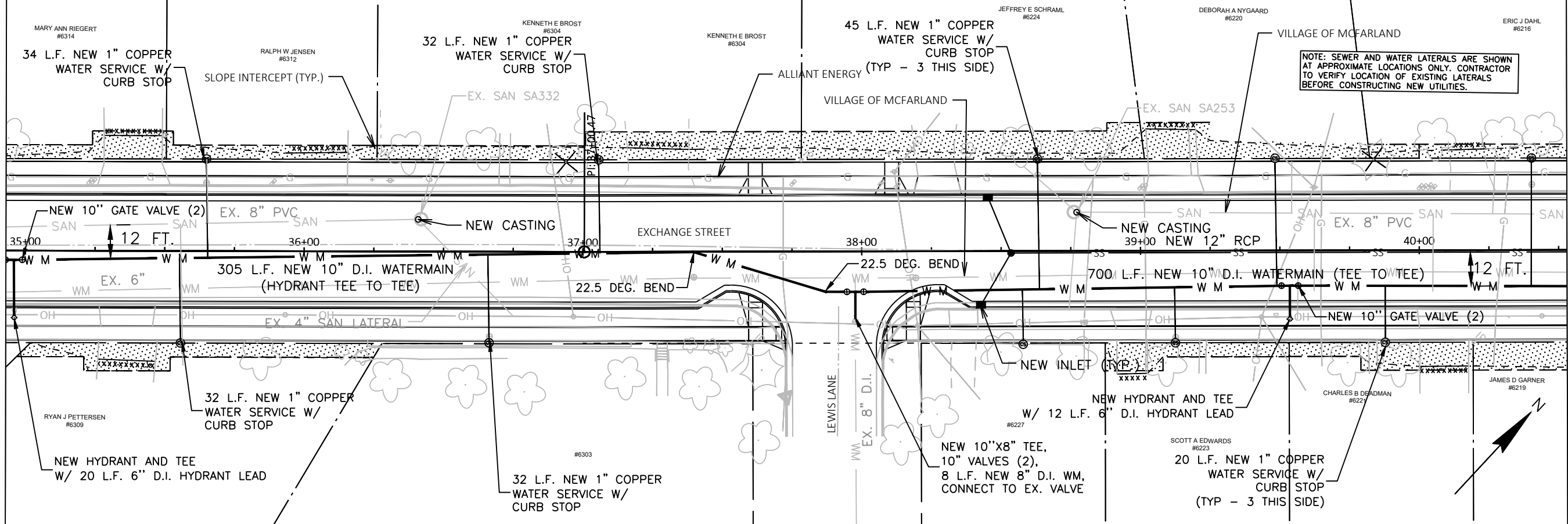




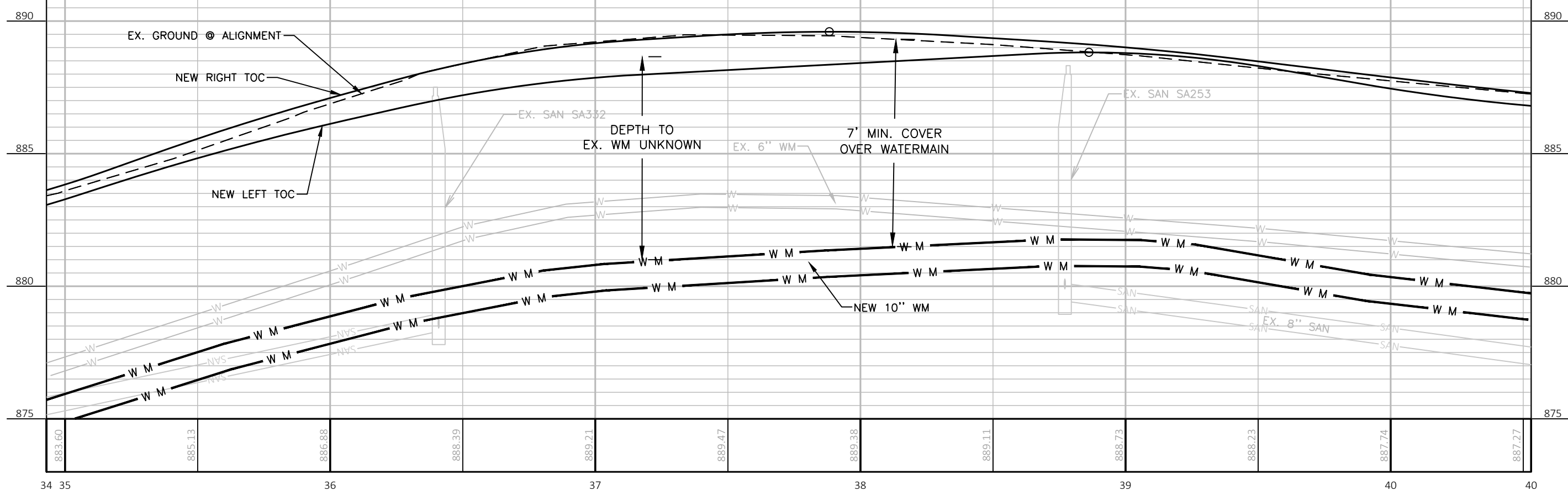


PROJECT NO: 5685-00-06	HWY: EXCHANGE STREET	COUNTY: DANE	EXCHANGE STREET WATERMAIN AND SANITARY SEWER DETAILS	SHEET 48	E
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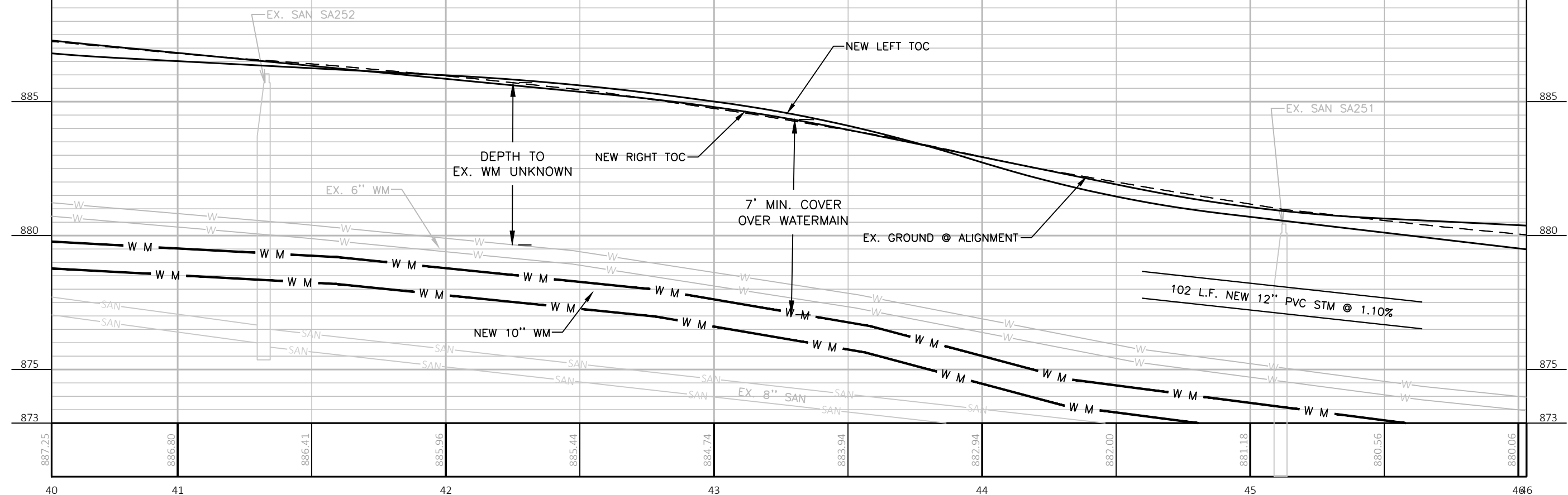
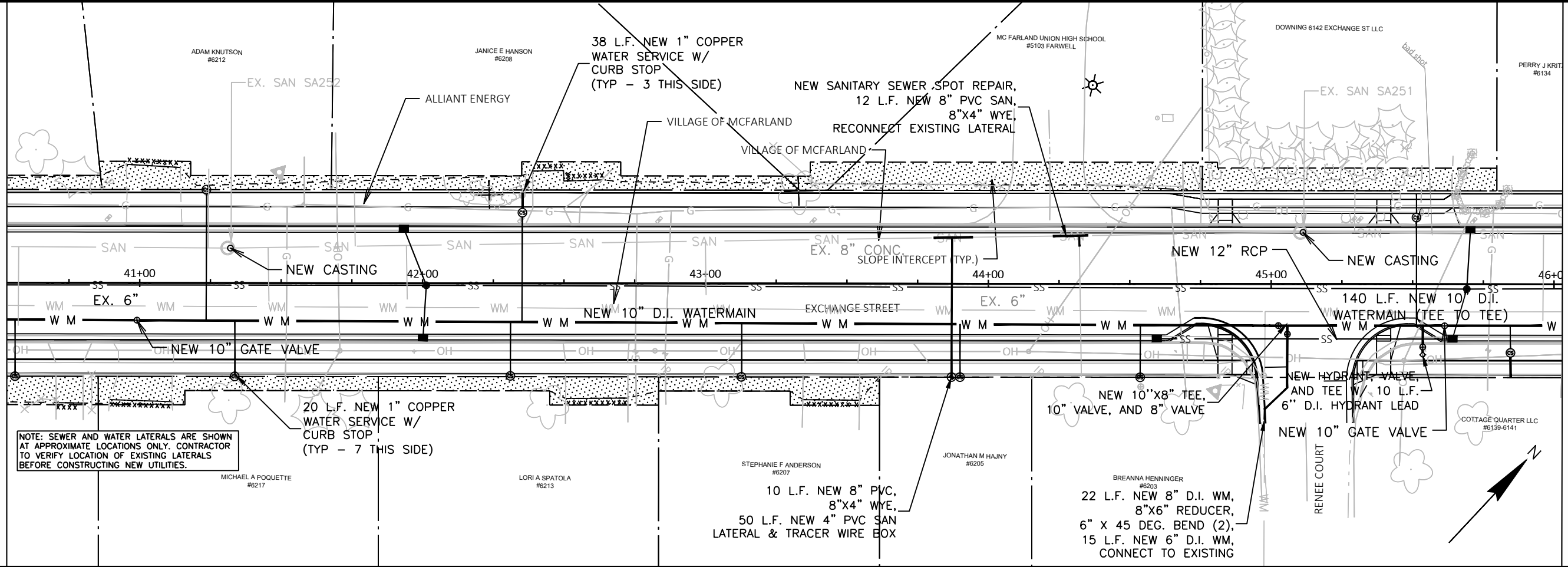


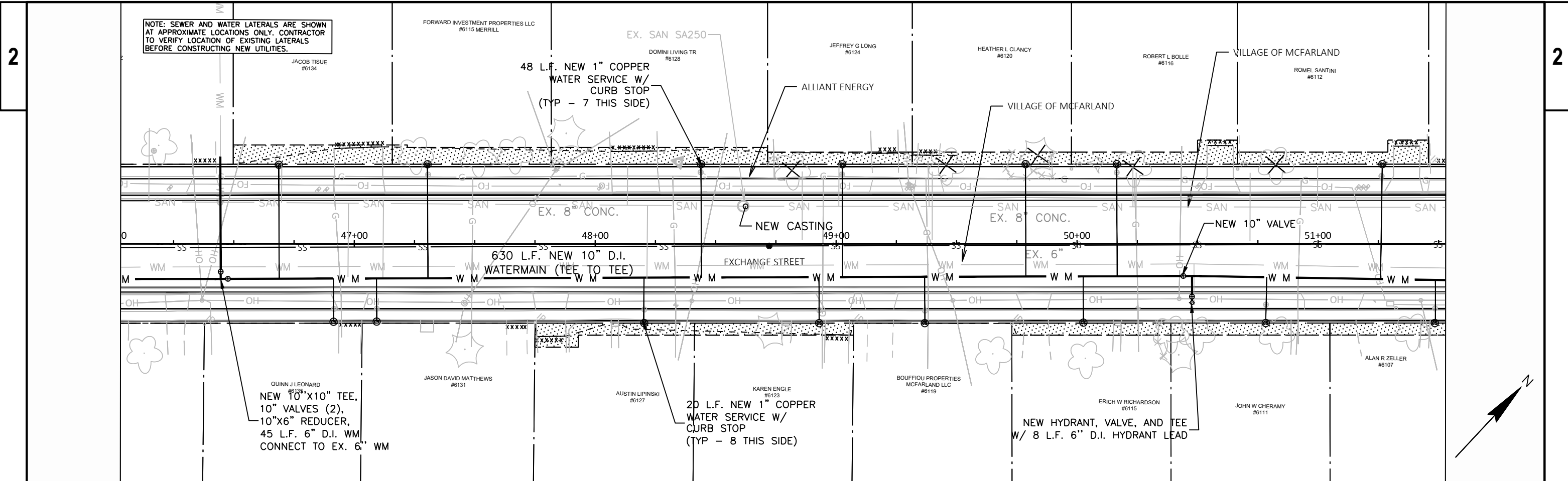


NOTE: SEWER AND WATER LATERALS ARE SHOWN AT APPROXIMATE LOCATIONS ONLY. CONTRACTOR TO VERIFY LOCATION OF EXISTING LATERALS BEFORE CONSTRUCTING NEW UTILITIES.

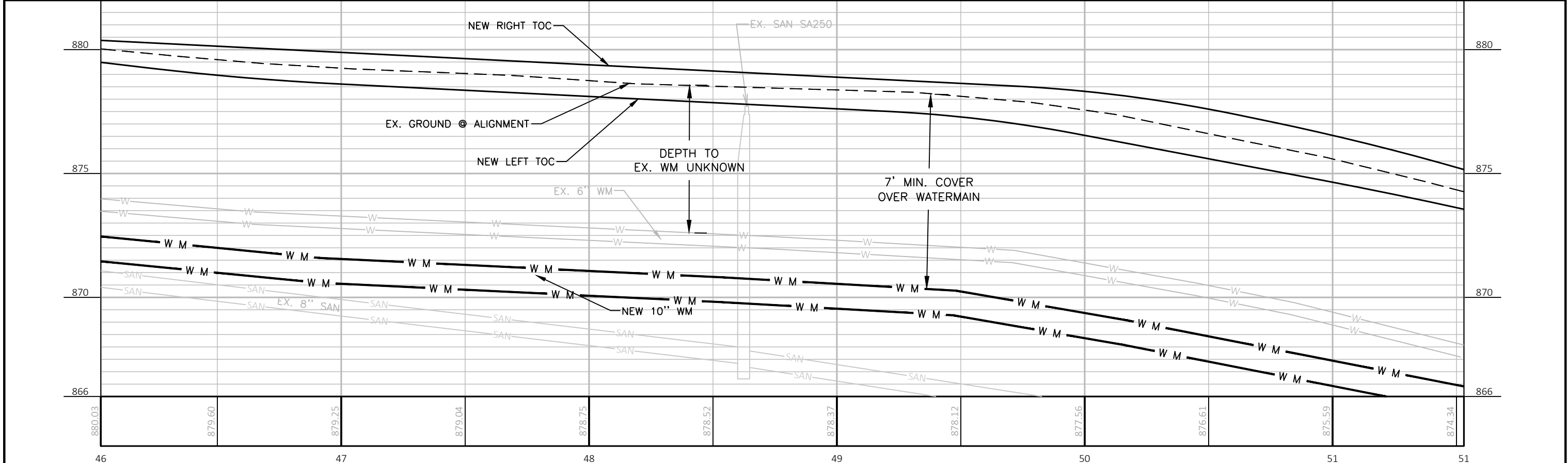
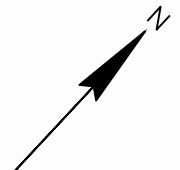


PROJECT NO: 5685-00-06	HWY: EXCHANGE STREET	COUNTY: DANE	EXCHANGE STREET WATERMAIN AND SANITARY SEWER DETAILS	SHEET 50	E
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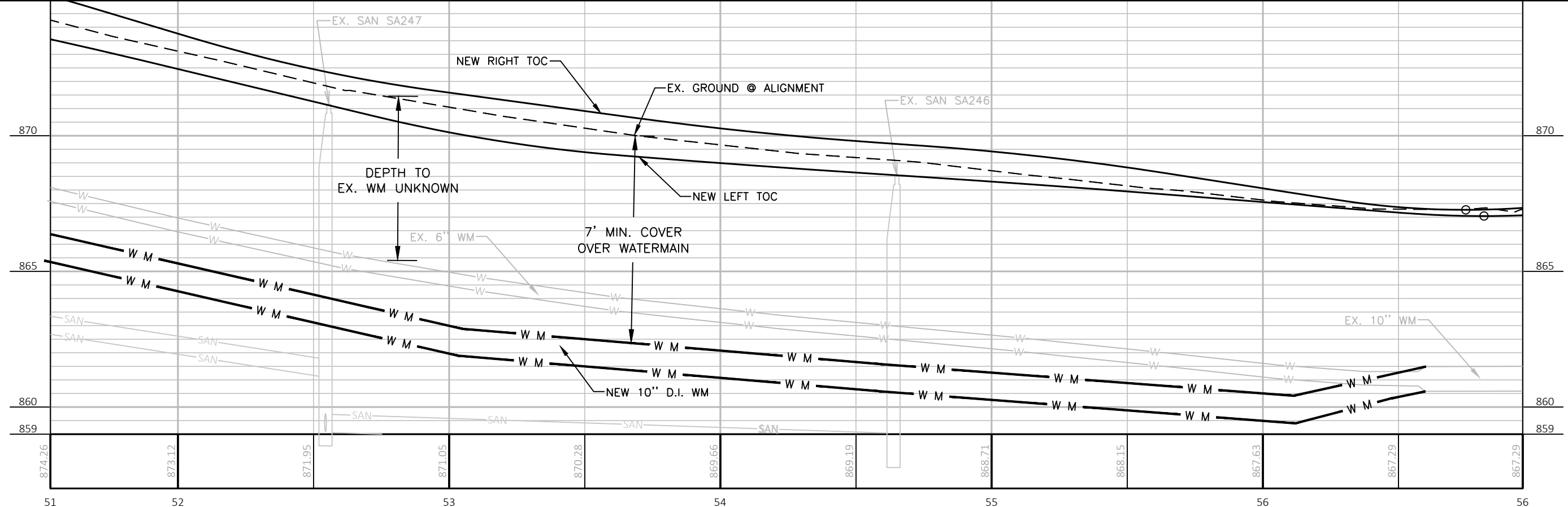
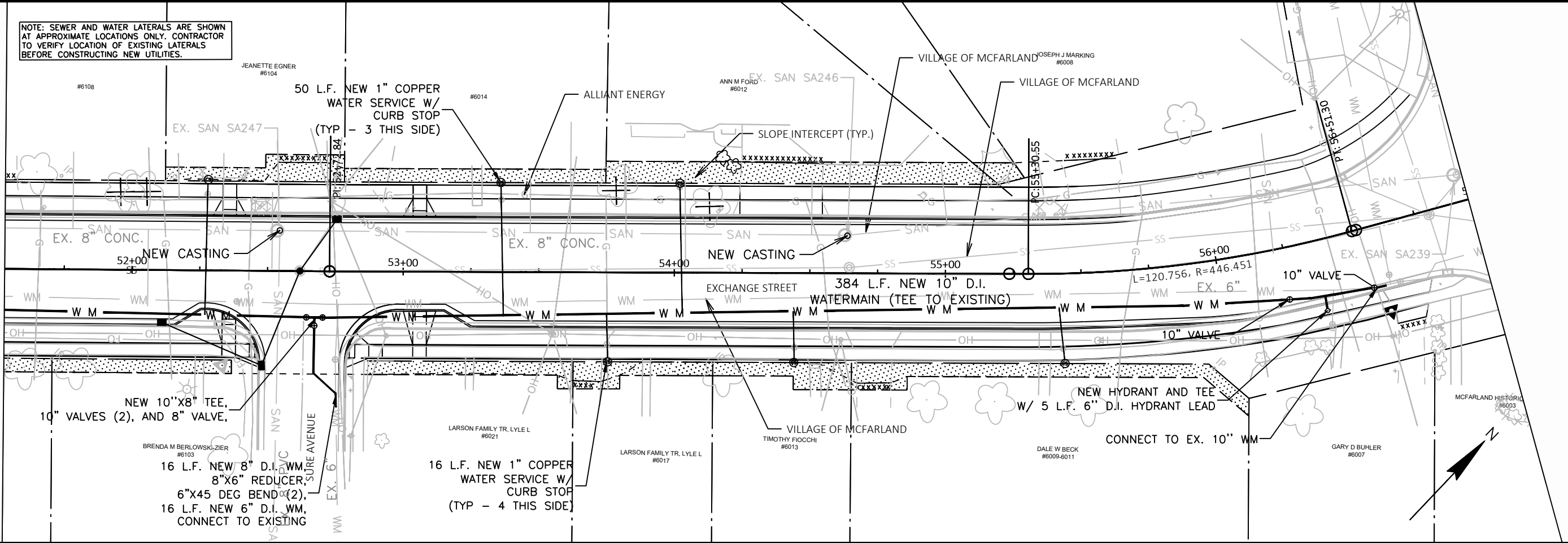


NOTE: SEWER AND WATER LATERALS ARE SHOWN AT APPROXIMATE LOCATIONS ONLY. CONTRACTOR TO VERIFY LOCATION OF EXISTING LATERALS BEFORE CONSTRUCTING NEW UTILITIES.

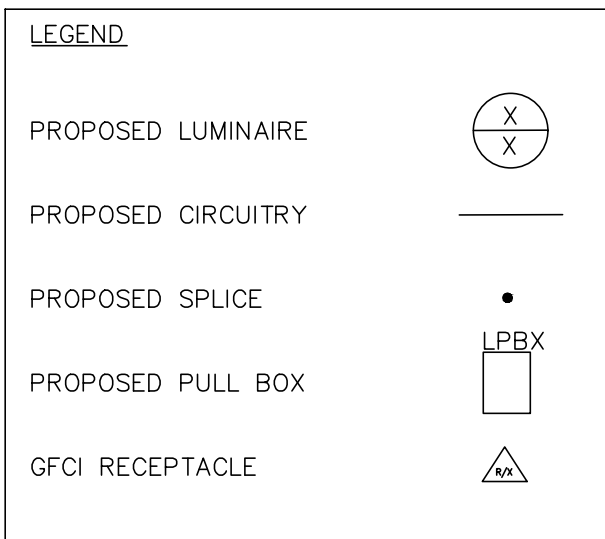


PROJECT NO: 5685-00-06	HWY: EXCHANGE STREET	COUNTY: DANE	EXCHANGE STREET WATERMAIN AND SANITARY SEWER DETAILS	SHEET 52	E
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NOTE: SEWER AND WATER LATERALS ARE SHOWN AT APPROXIMATE LOCATIONS ONLY. CONTRACTOR TO VERIFY LOCATION OF EXISTING LATERALS BEFORE CONSTRUCTING NEW UTILITIES.

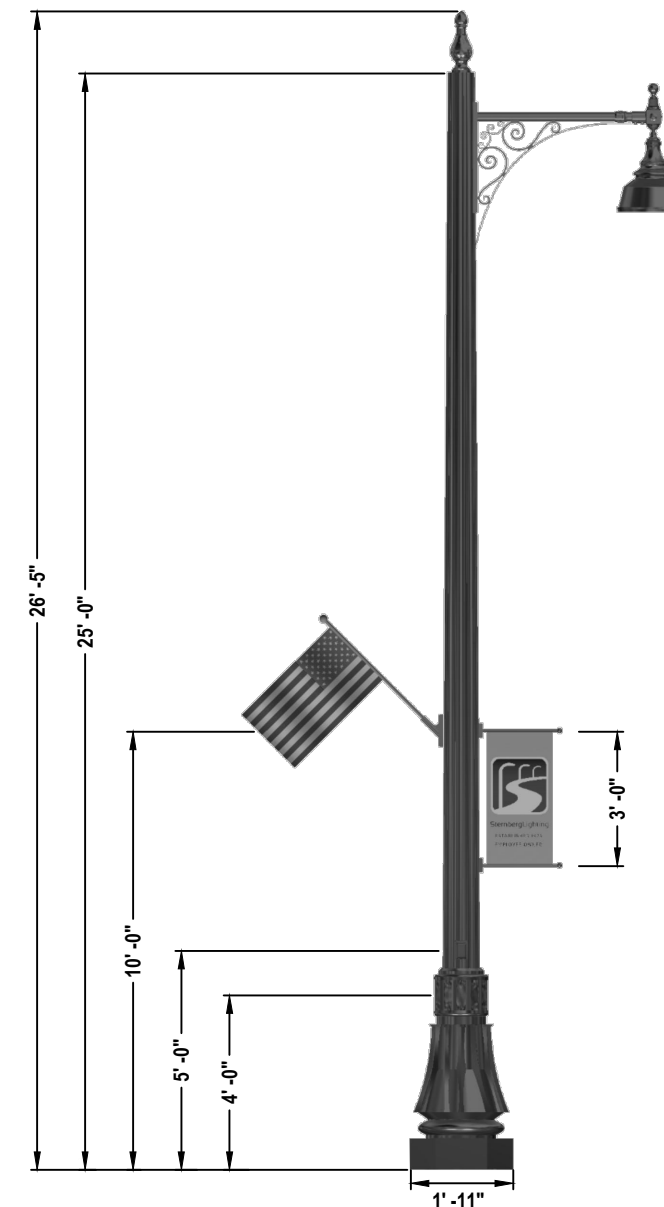
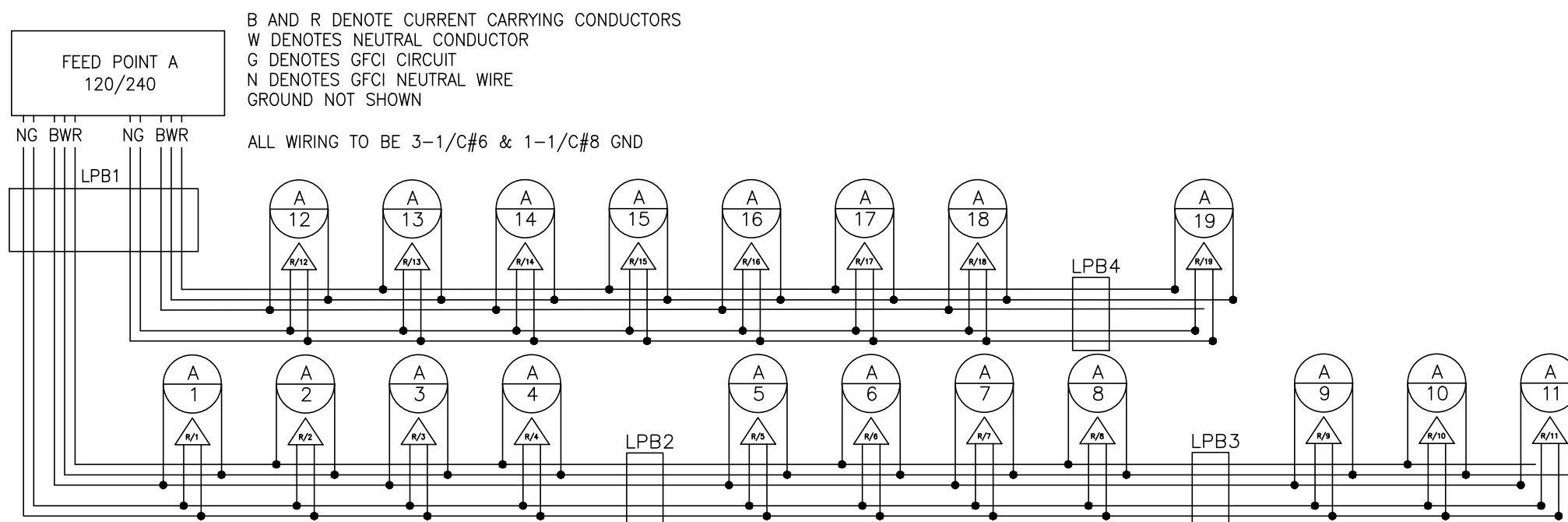


PROJECT NO: 5685-00-06	HWY: EXCHANGE STREET	COUNTY: DANE	EXCHANGE STREET WATERMAIN AND SANITARY SEWER DETAILS	SHEET 53	E
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LIGHTING UNIT EQUIPMENT TABLE	
ITEM	ITEM DESCRIPTION
SPV.0060.18	LIGHTING UNIT 1
SPV.0060.19	LIGHTING UNIT 2
652.0225	CONDUIT RIGID NONMETALLIC SCHEDULE 40 2-INCH
653.0154	PULL BOXES NON-CONDUCTIVE 24x26-INCH
655.0625	ELECTRIC WIRE LIGHTING 6 AWG
659.2124	LIGHTING CONTROL CABINETS 120/240V 24-IN

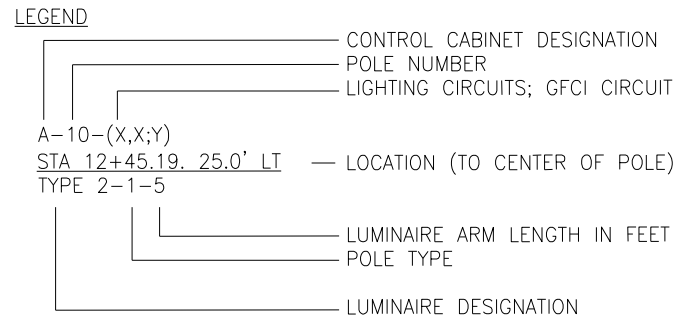
LUMINAIRE DESIGNATION			
NUMBER	TYPE	WATTS	DESCRIPTION
1	1940LED-1L40T2-MDL06-CA	36.6	LIGHTING UNIT 1
2	1940LED-1L40T3-MDL12-CA	68.7	LIGHTING UNIT 2



LIGHT POLE DETAIL
NOT TO SCALE

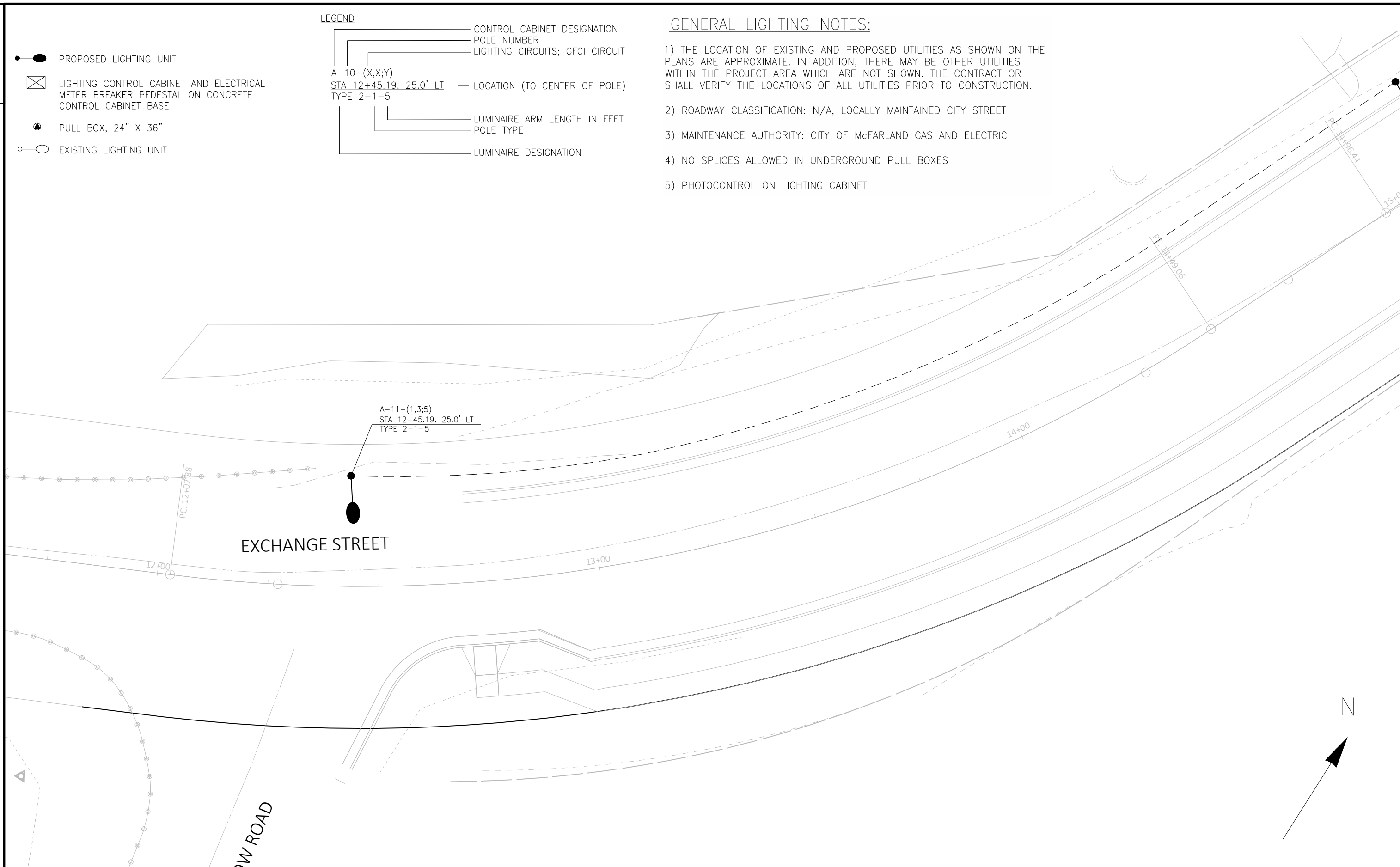
Hamilton Series 64-25-A-RTF-12FF-RDBP-GFI-FH-DBA HB BKT
with a SMA 4'-6"

- PROPOSED LIGHTING UNIT
- ⊠ LIGHTING CONTROL CABINET AND ELECTRICAL METER BREAKER PEDESTAL ON CONCRETE CONTROL CABINET BASE
- PULL BOX, 24" X 36"
- EXISTING LIGHTING UNIT



GENERAL LIGHTING NOTES:

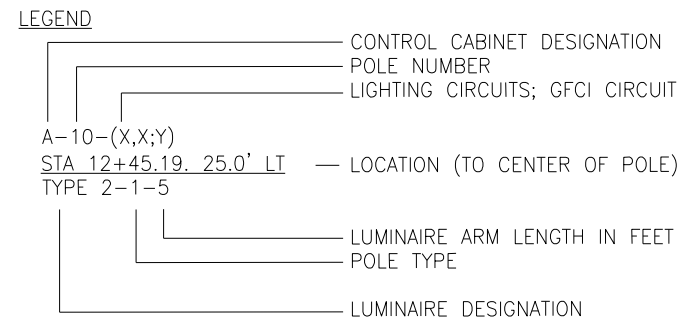
- 1) THE LOCATION OF EXISTING AND PROPOSED UTILITIES AS SHOWN ON THE PLANS ARE APPROXIMATE. IN ADDITION, THERE MAY BE OTHER UTILITIES WITHIN THE PROJECT AREA WHICH ARE NOT SHOWN. THE CONTRACTOR SHALL VERIFY THE LOCATIONS OF ALL UTILITIES PRIOR TO CONSTRUCTION.
- 2) ROADWAY CLASSIFICATION: N/A, LOCALLY MAINTAINED CITY STREET
- 3) MAINTENANCE AUTHORITY: CITY OF McFARLAND GAS AND ELECTRIC
- 4) NO SPLICES ALLOWED IN UNDERGROUND PULL BOXES
- 5) PHOTOCONTROL ON LIGHTING CABINET



2

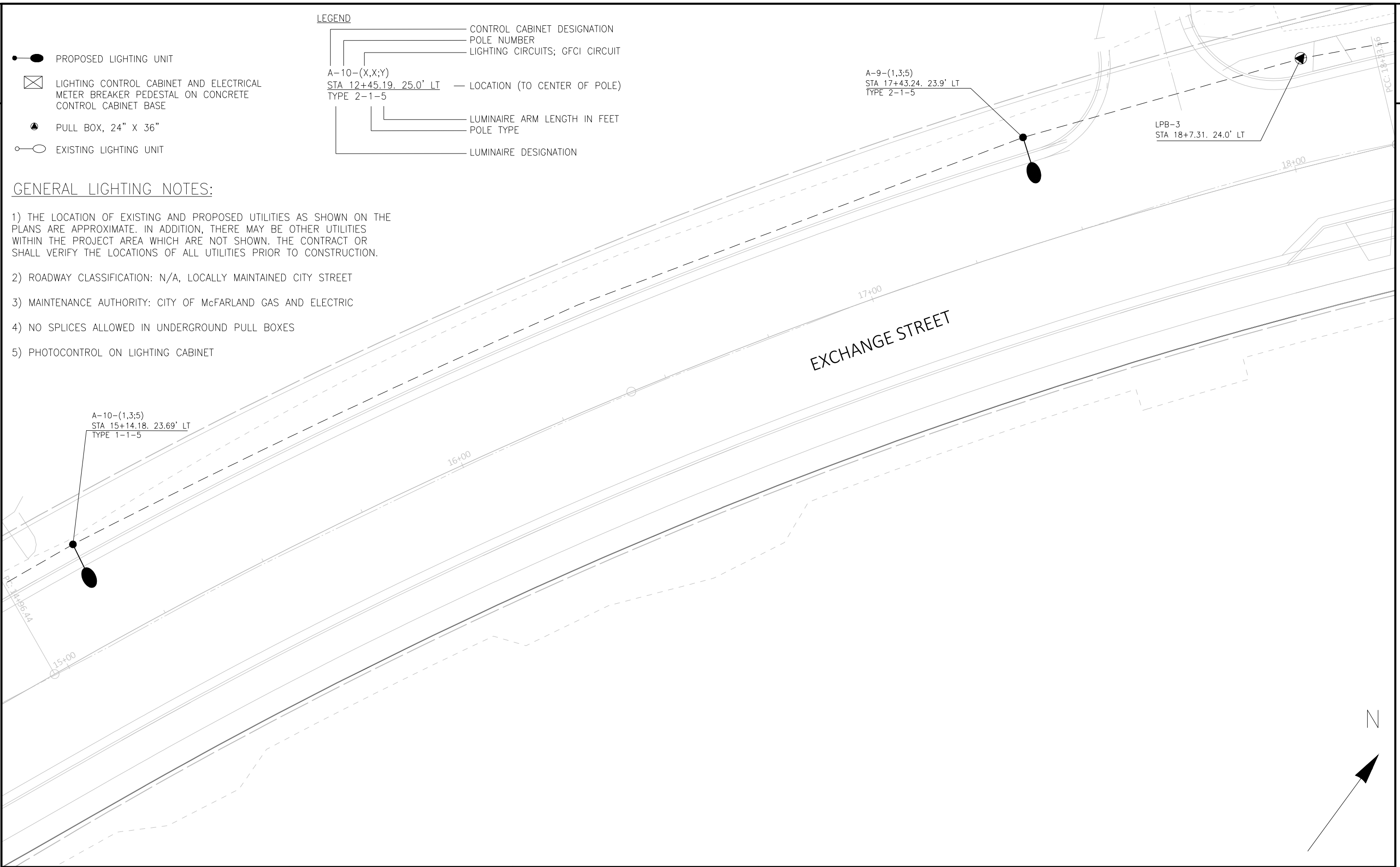
2

- PROPOSED LIGHTING UNIT
- ⊠ LIGHTING CONTROL CABINET AND ELECTRICAL METER BREAKER PEDESTAL ON CONCRETE CONTROL CABINET BASE
- PULL BOX, 24" X 36"
- EXISTING LIGHTING UNIT

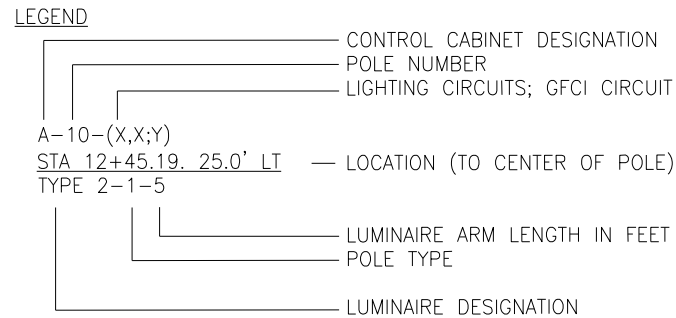


GENERAL LIGHTING NOTES:

- 1) THE LOCATION OF EXISTING AND PROPOSED UTILITIES AS SHOWN ON THE PLANS ARE APPROXIMATE. IN ADDITION, THERE MAY BE OTHER UTILITIES WITHIN THE PROJECT AREA WHICH ARE NOT SHOWN. THE CONTRACTOR SHALL VERIFY THE LOCATIONS OF ALL UTILITIES PRIOR TO CONSTRUCTION.
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- 3) MAINTENANCE AUTHORITY: CITY OF MCFARLAND GAS AND ELECTRIC
- 4) NO SPLICES ALLOWED IN UNDERGROUND PULL BOXES
- 5) PHOTOCONTROL ON LIGHTING CABINET

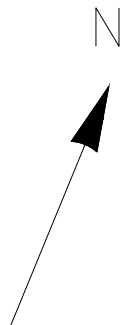


- PROPOSED LIGHTING UNIT
- ⊠ LIGHTING CONTROL CABINET AND ELECTRICAL METER BREAKER PEDESTAL ON CONCRETE CONTROL CABINET BASE
- PULL BOX, 24" X 36"
- EXISTING LIGHTING UNIT

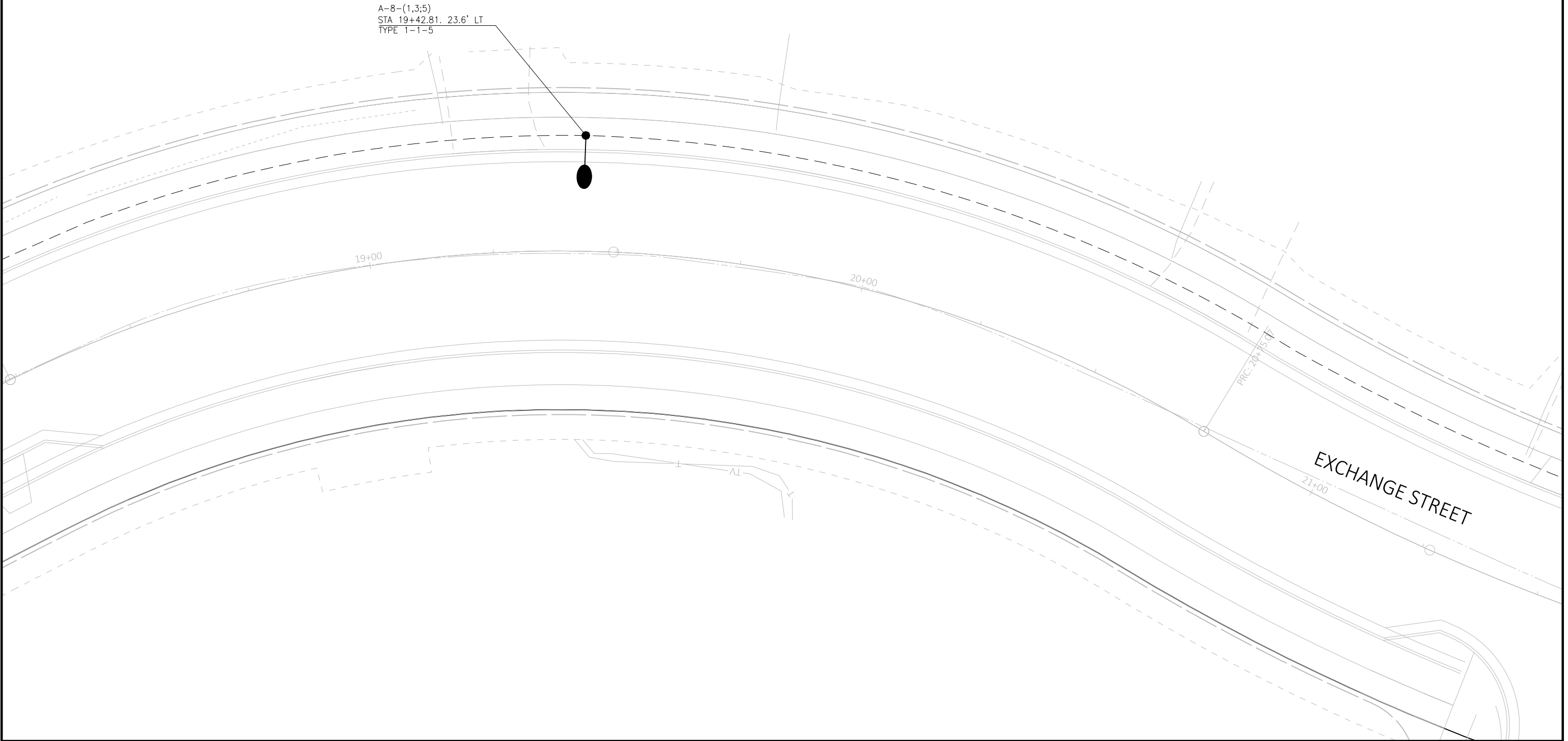


GENERAL LIGHTING NOTES:

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- 2) ROADWAY CLASSIFICATION: N/A, LOCALLY MAINTAINED CITY STREET
- 3) MAINTENANCE AUTHORITY: CITY OF McFARLAND GAS AND ELECTRIC
- 4) NO SPLICES ALLOWED IN UNDERGROUND PULL BOXES
- 5) PHOTOCONTROL ON LIGHTING CABINET



A-8-(1,3;5)
STA 19+42.81. 23.6' LT
TYPE 1-1-5



- PROPOSED LIGHTING UNIT
- ⊠ LIGHTING CONTROL CABINET AND ELECTRICAL METER BREAKER PEDESTAL ON CONCRETE CONTROL CABINET BASE
- PULL BOX, 24" X 36"
- EXISTING LIGHTING UNIT

- LEGEND**
- CONTROL CABINET DESIGNATION
 - POLE NUMBER
 - LIGHTING CIRCUITS; GFCI CIRCUIT
 - A-10-(X,X;Y)
STA 12+45.19. 25.0' LT
TYPE 2-1-5 — LOCATION (TO CENTER OF POLE)
 - LUMINAIRE ARM LENGTH IN FEET
 - POLE TYPE
 - LUMINAIRE DESIGNATION

GENERAL LIGHTING NOTES:

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- 5) PHOTOCONTROL ON LIGHTING CABINET

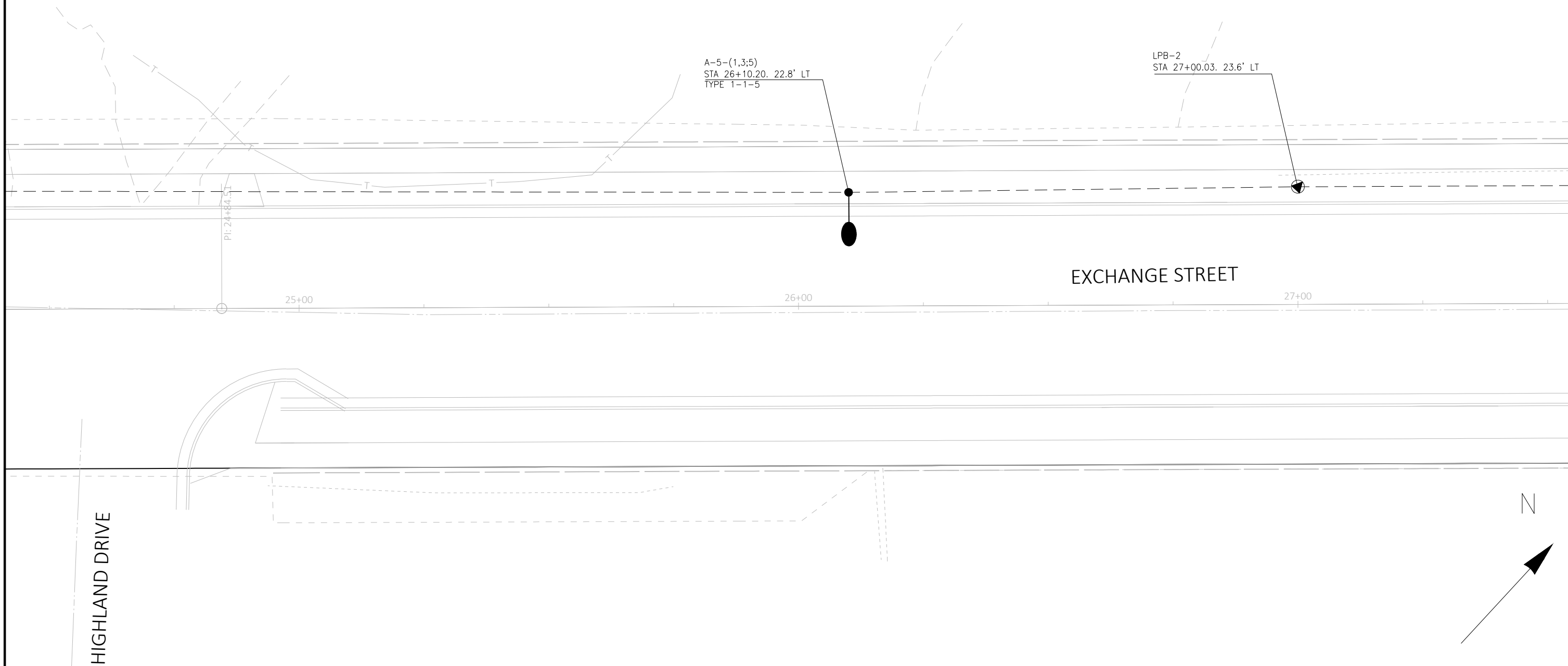






- PROPOSED LIGHTING UNIT
- ⊠ LIGHTING CONTROL CABINET AND ELECTRICAL METER BREAKER PEDESTAL ON CONCRETE CONTROL CABINET BASE
- PULL BOX, 24" X 36"
- EXISTING LIGHTING UNIT

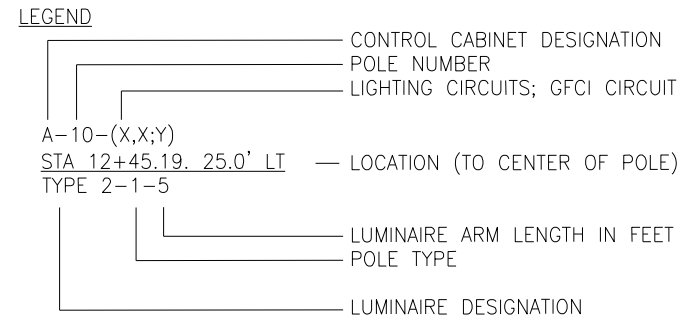
- LEGEND**
- CONTROL CABINET DESIGNATION
 - POLE NUMBER
 - LIGHTING CIRCUITS; GFCI CIRCUIT
 - A-10-(X,X;Y)
STA 12+45.19. 25.0' LT — LOCATION (TO CENTER OF POLE)
TYPE 2-1-5
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 - POLE TYPE
 - LUMINAIRE DESIGNATION

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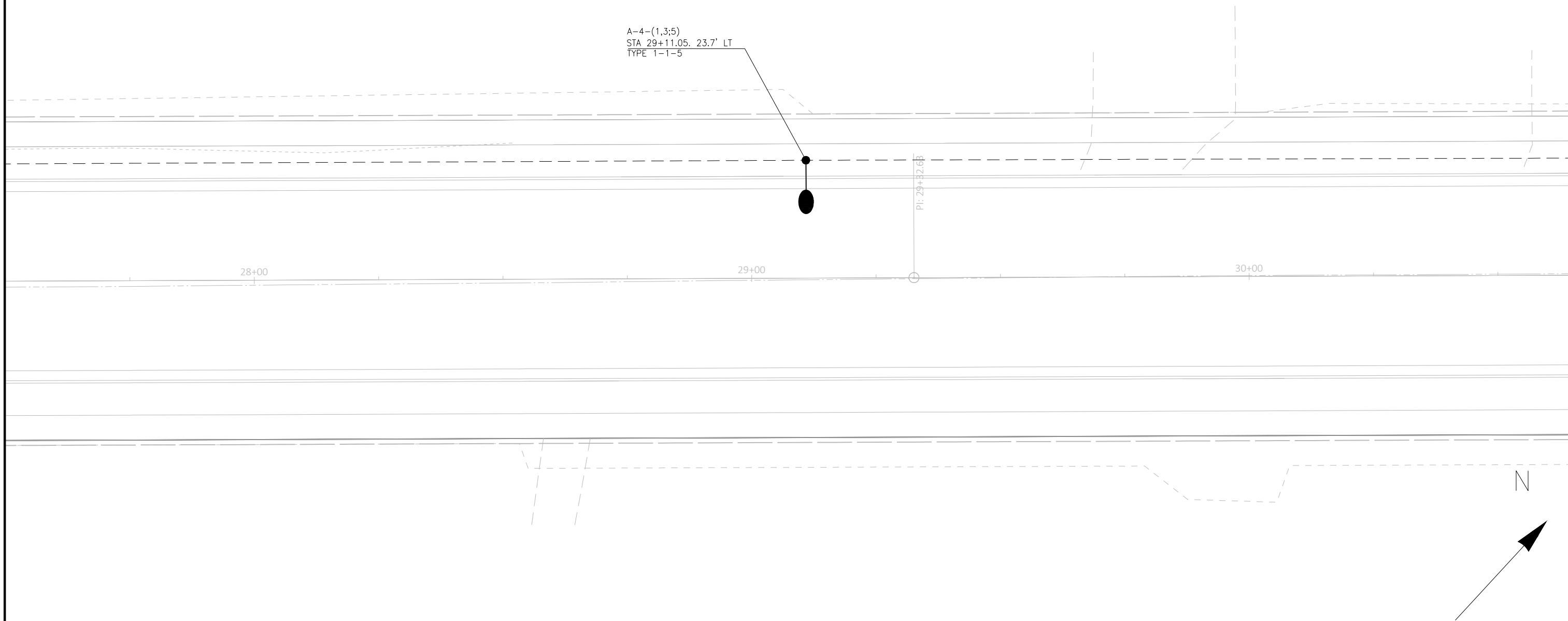






-  PROPOSED LIGHTING UNIT
-  LIGHTING CONTROL CABINET AND ELECTRICAL METER BREAKER PEDESTAL ON CONCRETE CONTROL CABINET BASE
-  PULL BOX, 24" X 36"
-  EXISTING LIGHTING UNIT

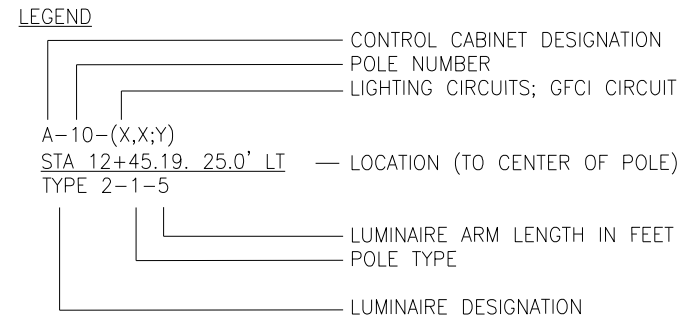


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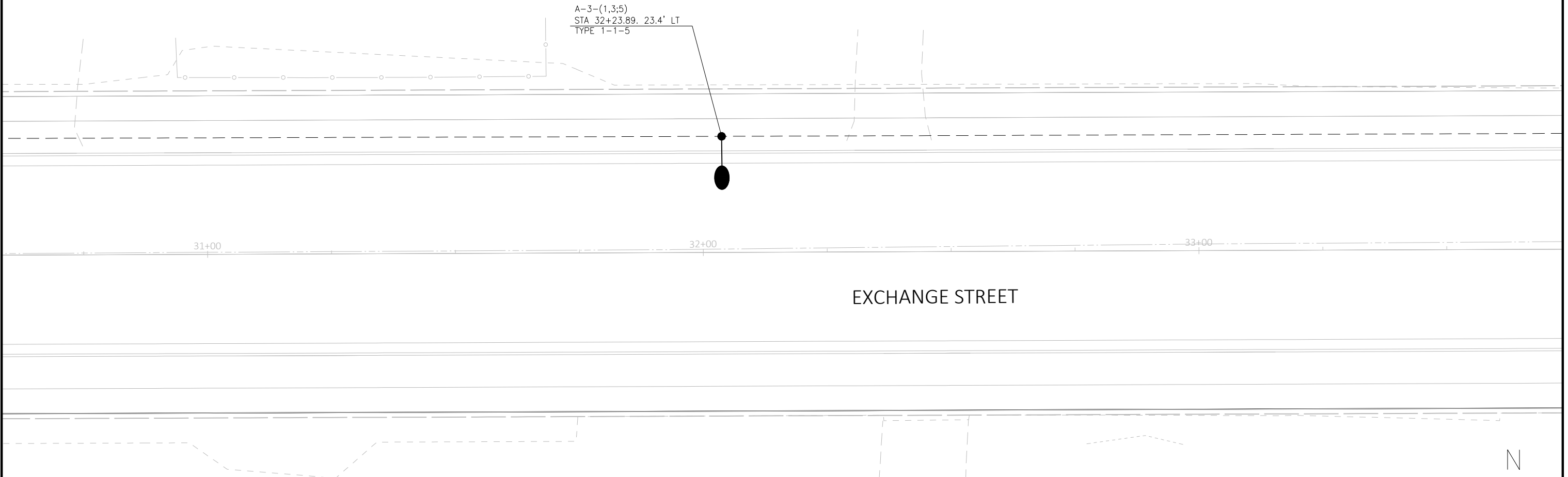
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-  LIGHTING CONTROL CABINET AND ELECTRICAL METER BREAKER PEDESTAL ON CONCRETE CONTROL CABINET BASE
-  PULL BOX, 24" X 36"
-  EXISTING LIGHTING UNIT



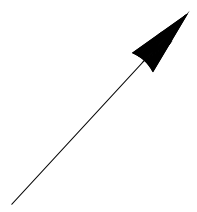
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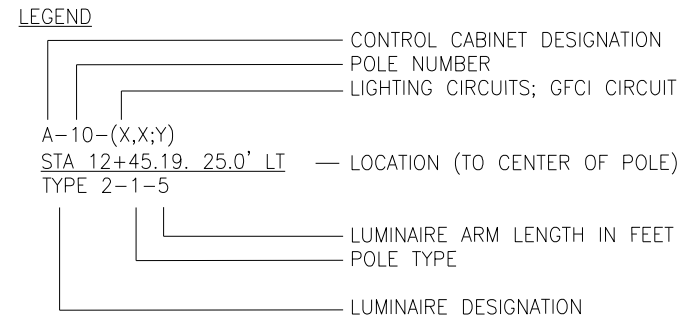
A-3-(1,3;5)
 STA 32+23.89. 23.4' LT
 TYPE 1-1-5



N

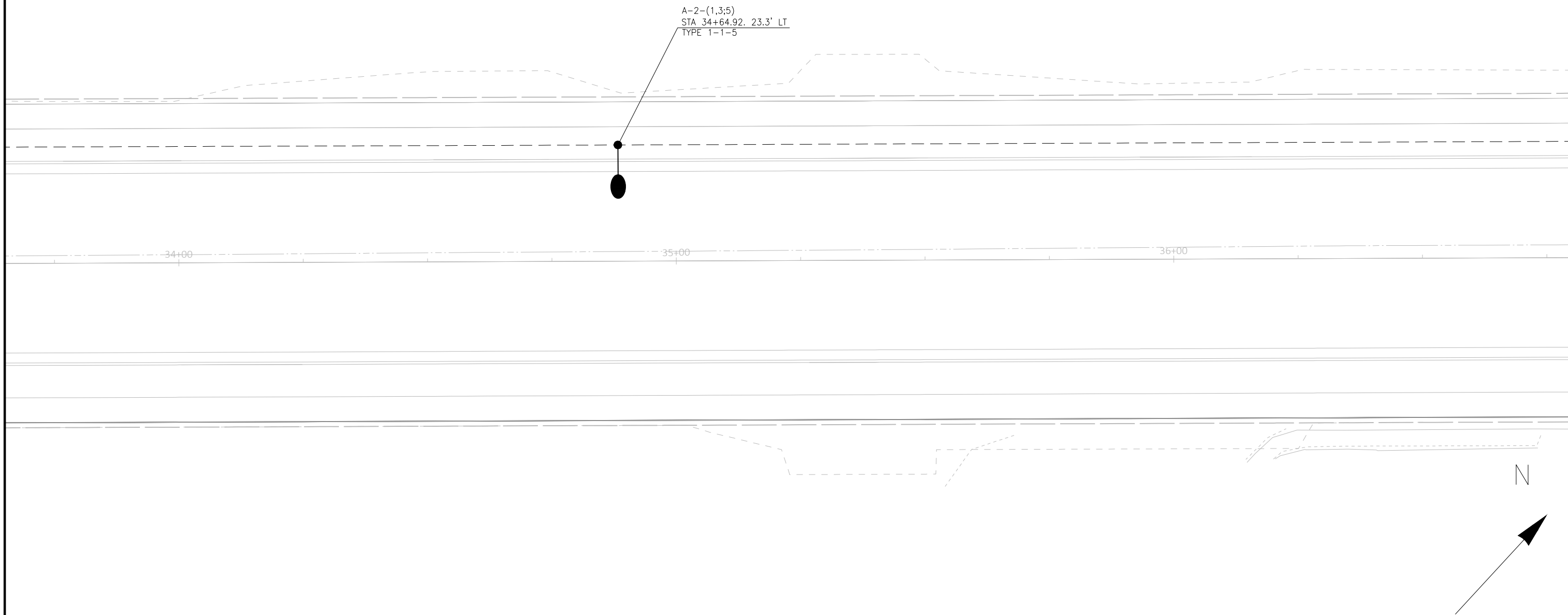


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- ⊠ LIGHTING CONTROL CABINET AND ELECTRICAL METER BREAKER PEDESTAL ON CONCRETE CONTROL CABINET BASE
- PULL BOX, 24" X 36"
- EXISTING LIGHTING UNIT

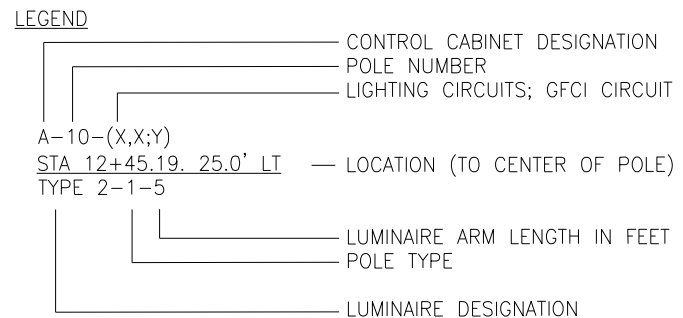


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- 5) PHOTOCONTROL ON LIGHTING CABINET

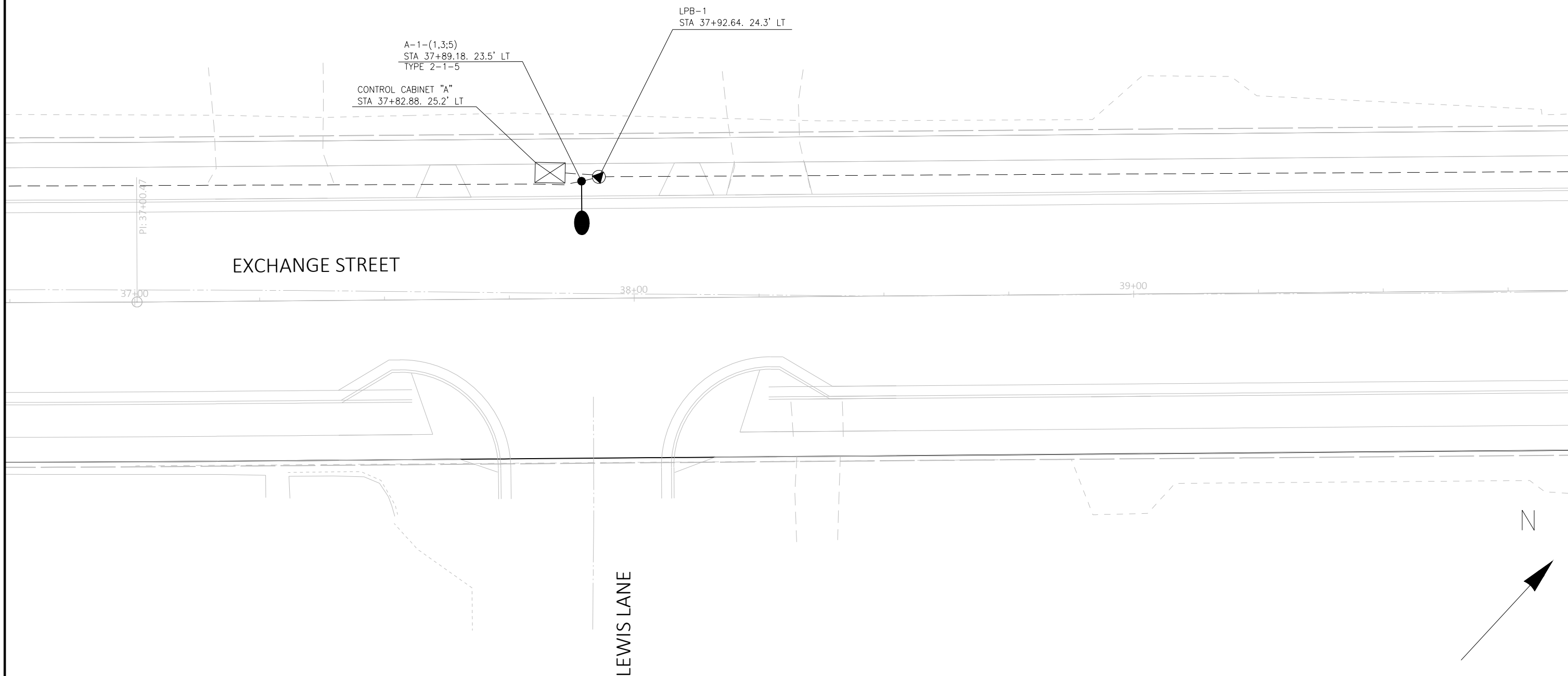


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- PULL BOX, 24" X 36"
- EXISTING LIGHTING UNIT

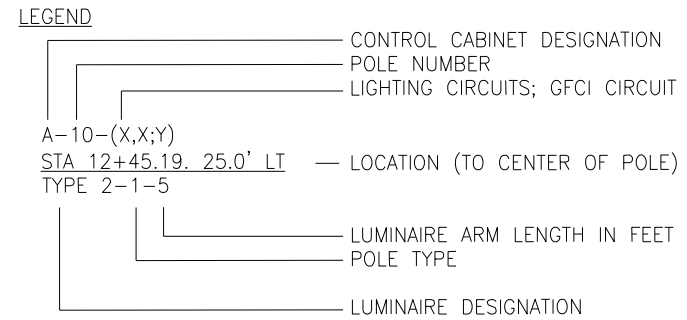


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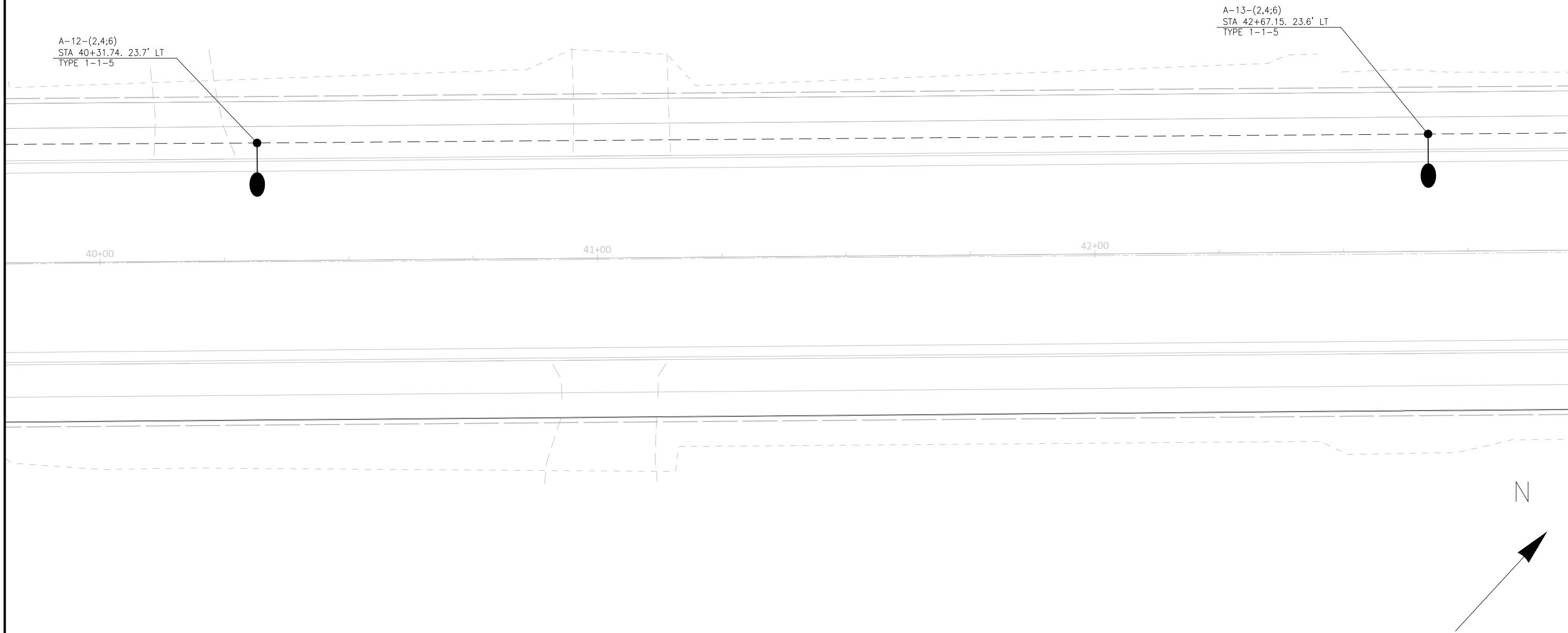


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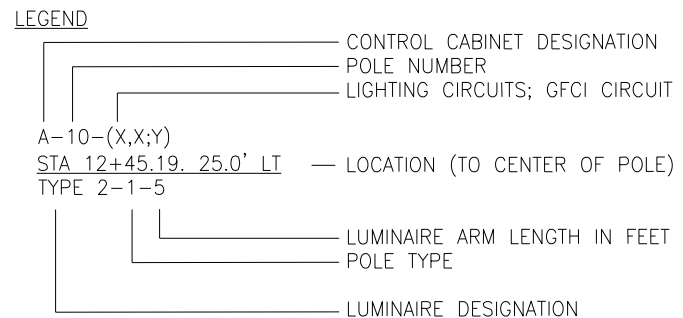


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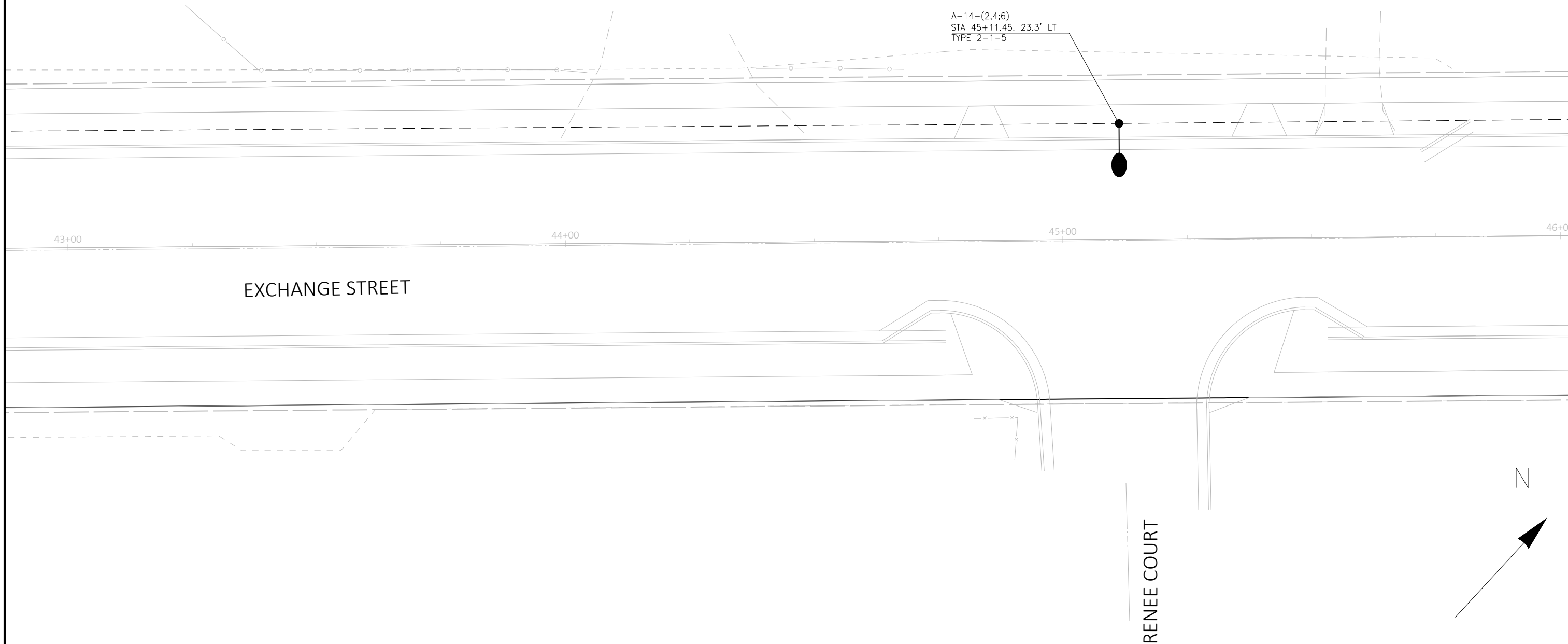






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- PULL BOX, 24" X 36"
- EXISTING LIGHTING UNIT

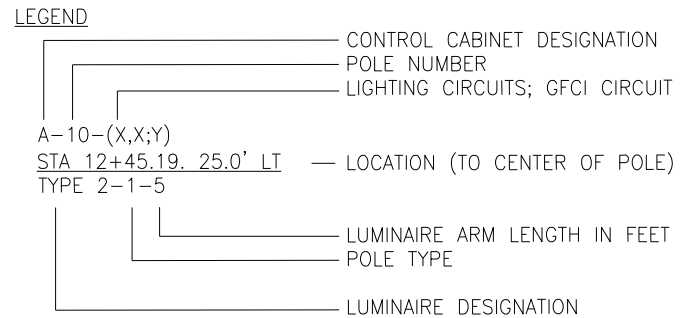


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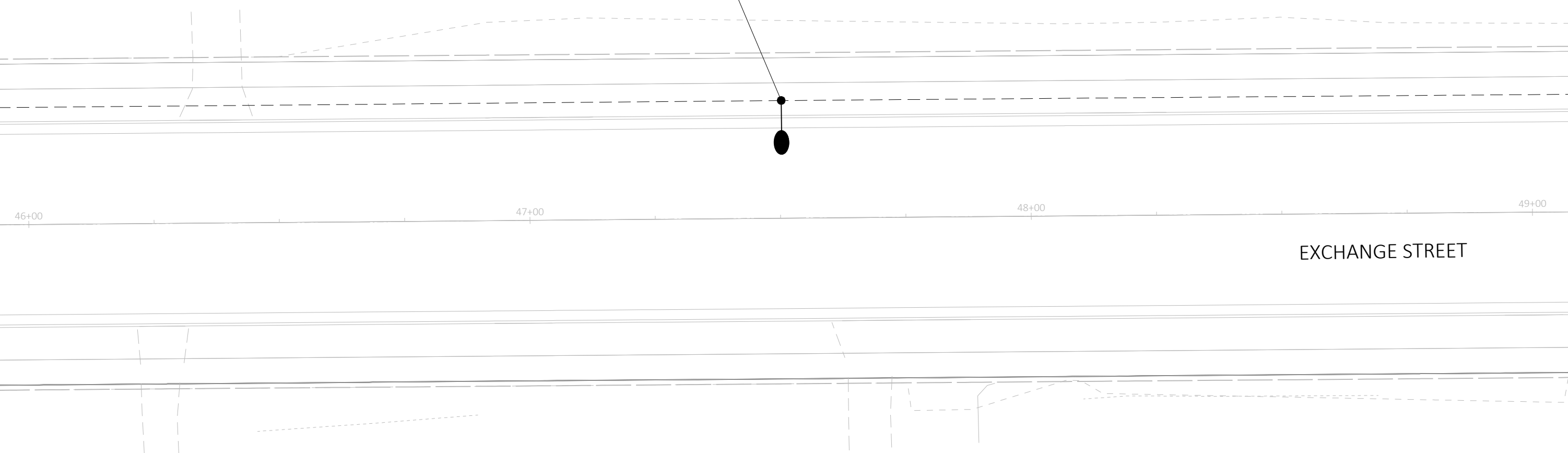
-  PROPOSED LIGHTING UNIT
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-  PULL BOX, 24" X 36"
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GENERAL LIGHTING NOTES:





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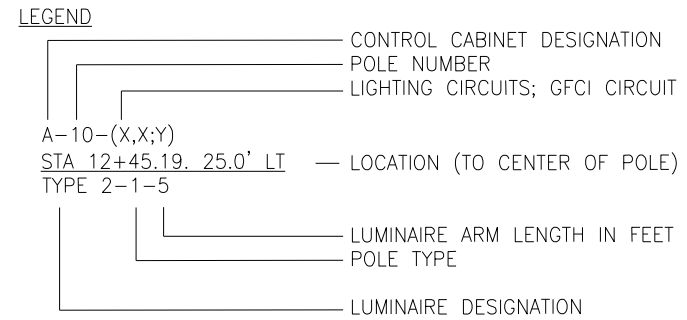
A-15-(2,4;6)
 STA 47+50.30, 23.5' LT
 TYPE 1-1-5



EXCHANGE STREET

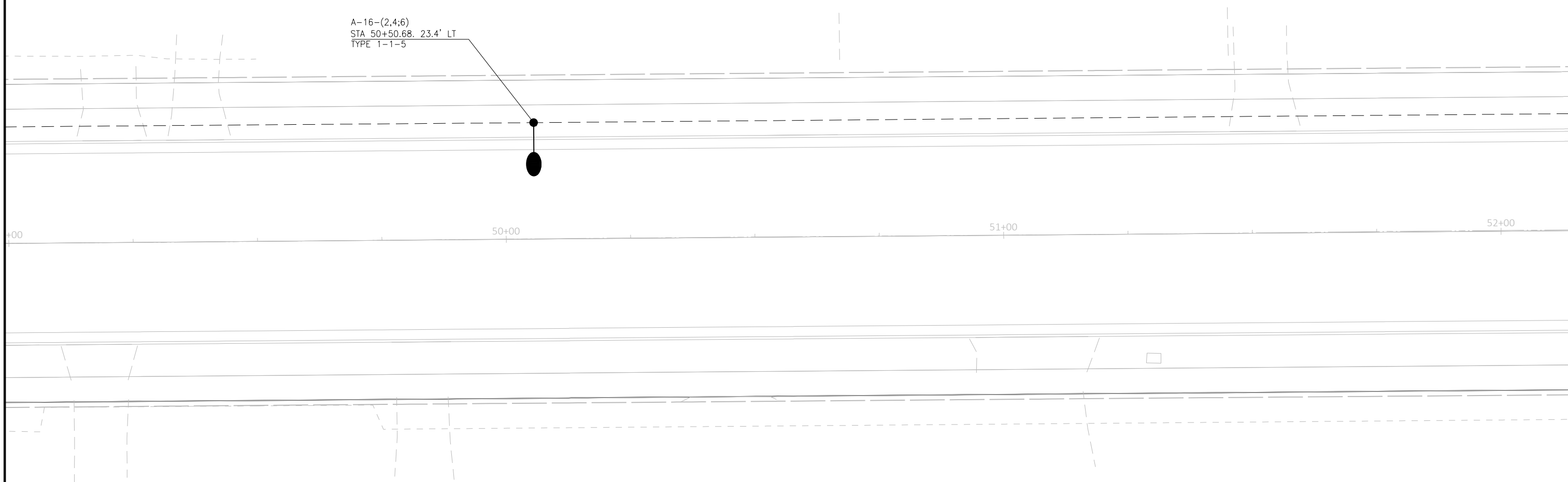
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-  PROPOSED LIGHTING UNIT
-  LIGHTING CONTROL CABINET AND ELECTRICAL METER BREAKER PEDESTAL ON CONCRETE CONTROL CABINET BASE
-  PULL BOX, 24" X 36"
-  EXISTING LIGHTING UNIT

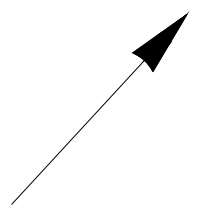


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- 5) PHOTOCONTROL ON LIGHTING CABINET



N

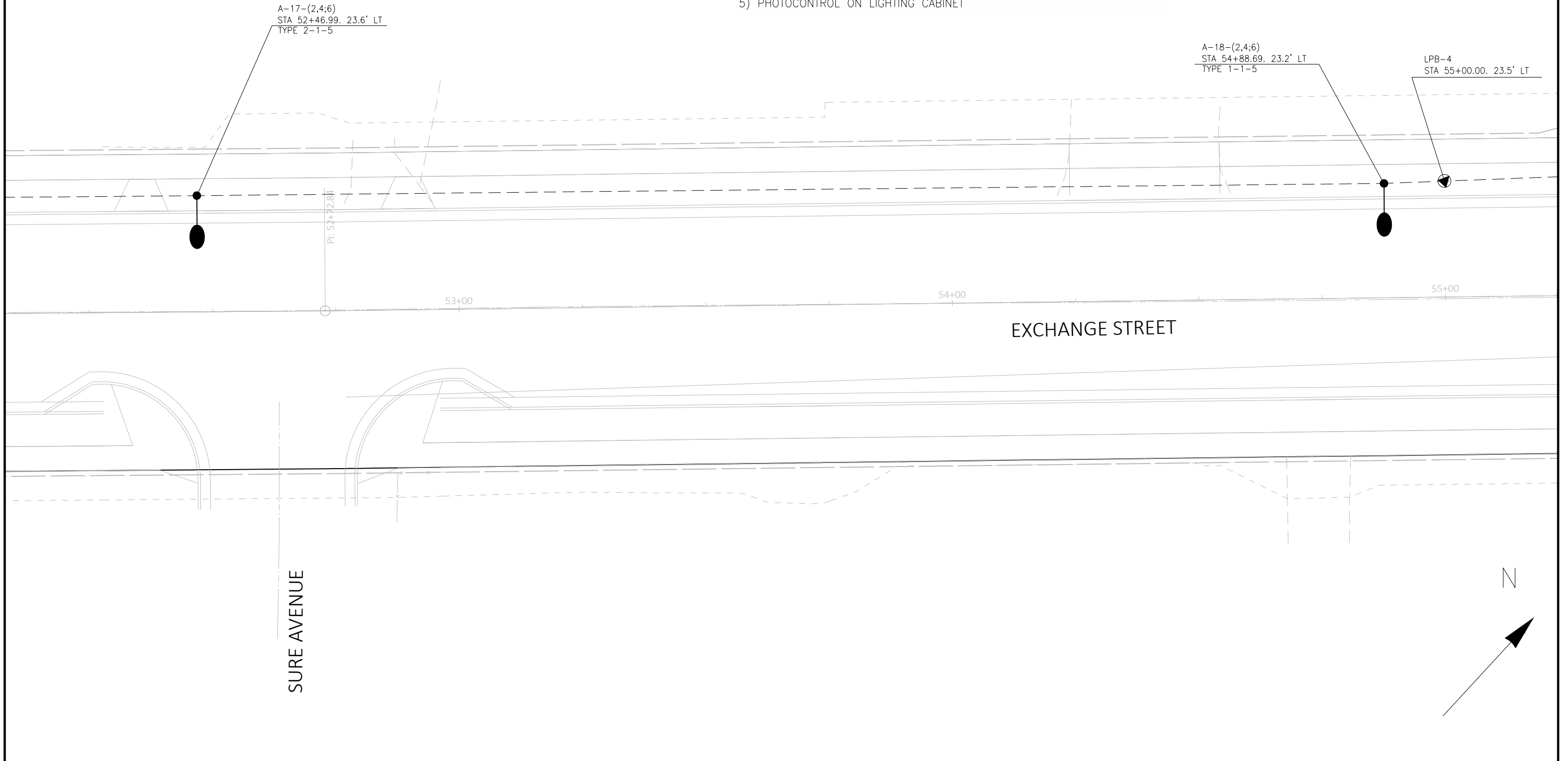


- PROPOSED LIGHTING UNIT
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- PULL BOX, 24" X 36"
- EXISTING LIGHTING UNIT

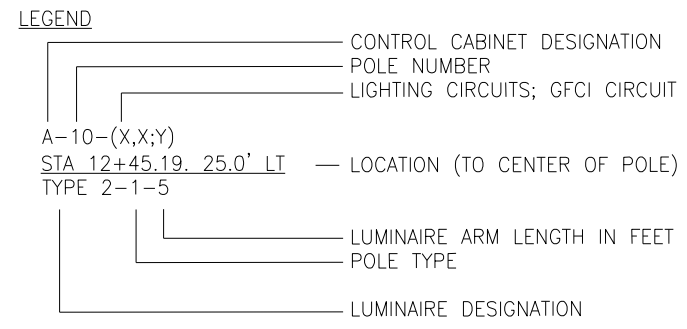
- LEGEND**
- CONTROL CABINET DESIGNATION
 - POLE NUMBER
 - LIGHTING CIRCUITS; GFCI CIRCUIT
 - A-10-(X,X;Y)
STA 12+45.19. 25.0' LT — LOCATION (TO CENTER OF POLE)
 - TYPE 2-1-5
 - LUMINAIRE ARM LENGTH IN FEET
 - POLE TYPE
 - LUMINAIRE DESIGNATION

GENERAL LIGHTING NOTES:

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- PULL BOX, 24" X 36"
- EXISTING LIGHTING UNIT












GENERAL LIGHTING NOTES:

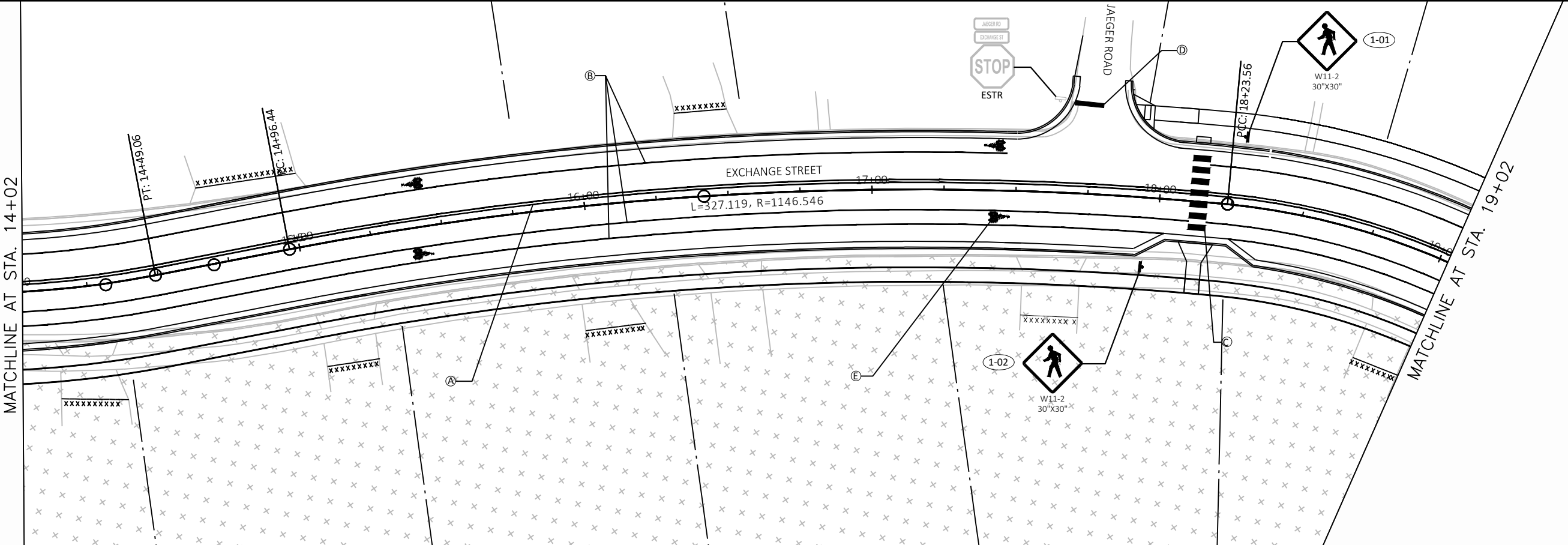
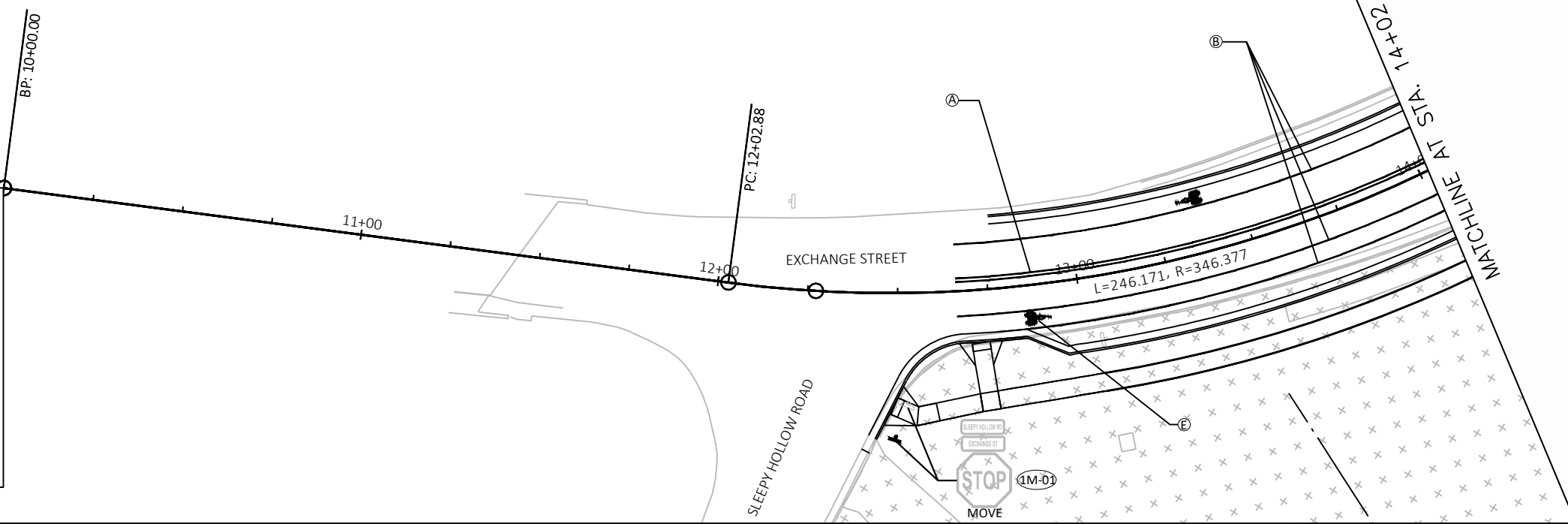
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- 5) PHOTOCONTROL ON LIGHTING CABINET

A-19-(2,4;6)
STA 56+87.16. 63.7' LT
TYPE 2-1-5



LEGEND

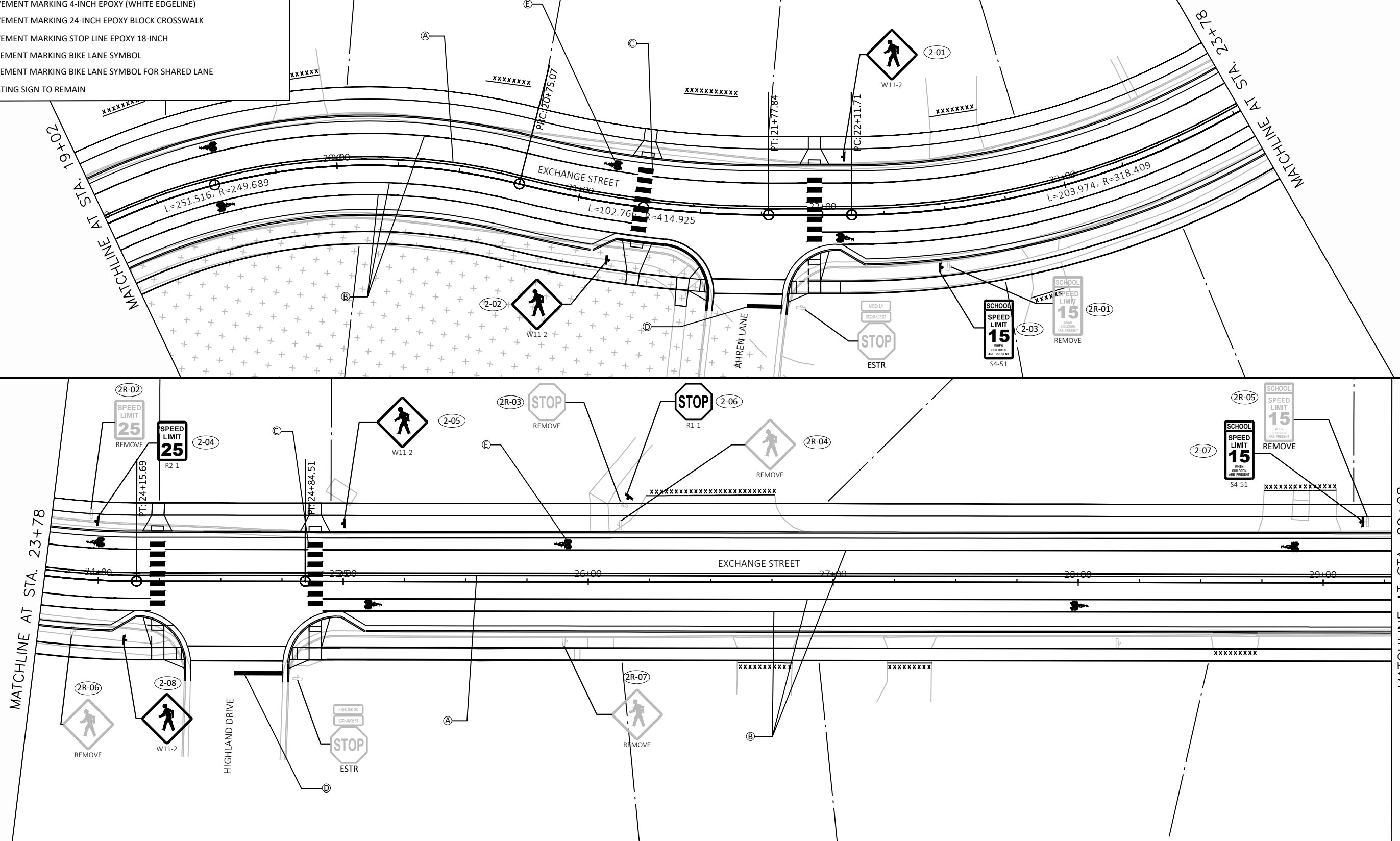
-  REMOVING SIGNS TYPE II/REMOVING SMALL SIGN SUPPORTS/MOVING SIGNS TYPE II/MOVING SMALL SIGN SUPPORTS
-  SIGNS TYPE II REFLECTIVE H AND F ON POSTS
-  PAVEMENT MARKING 4-INCH EPOXY (DOUBLE YELLOW)
-  PAVEMENT MARKING 4-INCH EPOXY (WHITE EDGELINE)
-  PAVEMENT MARKING 24-INCH EPOXY BLOCK CROSSWALK
-  PAVEMENT MARKING STOP LINE EPOXY 18-INCH
-  PAVEMENT MARKING BIKE LANE SYMBOL
-  PAVEMENT MARKING BIKE LANE SYMBOL FOR SHARED LANE
-  EXISTING SIGN TO REMAIN



PROJECT NO: 5685-00-05	HWY: EXCHANGE STREET	COUNTY: DANE	PERMANENT SIGNING/PAVEMENT MARKING PLAN	SHEET 70	E
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LEGEND

- REMOVING SIGNS TYPE II/REMOVING SMALL SIGN SUPPORTS/MOVING SIGNS TYPE II/MOVING SMALL SIGN SUPPORTS
- SIGNS TYPE II REFLECTIVE H AND F ON POSTS
- PAVEMENT MARKING 4-INCH EPOXY (DOUBLE YELLOW)
- PAVEMENT MARKING 4-INCH EPOXY (WHITE EDGELINE)
- PAVEMENT MARKING 24-INCH EPOXY BLOCK CROSSWALK
- PAVEMENT MARKING STOP LINE EPOXY 18-INCH
- PAVEMENT MARKING BIKE LANE SYMBOL
- PAVEMENT MARKING BIKE LANE SYMBOL FOR SHARED LANE
- ESTR EXISTING SIGN TO REMAIN



PROJECT NO: 5685-00-05

HWY: EXCHANGE STREET

COUNTY: DANE

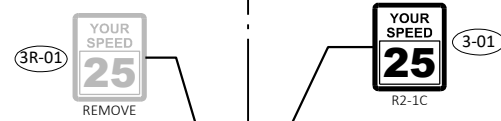
PERMANENT SIGNING/PAVEMENT MARKING PLAN

SHEET 71

E

MATCHLINE AT STA. 29+28

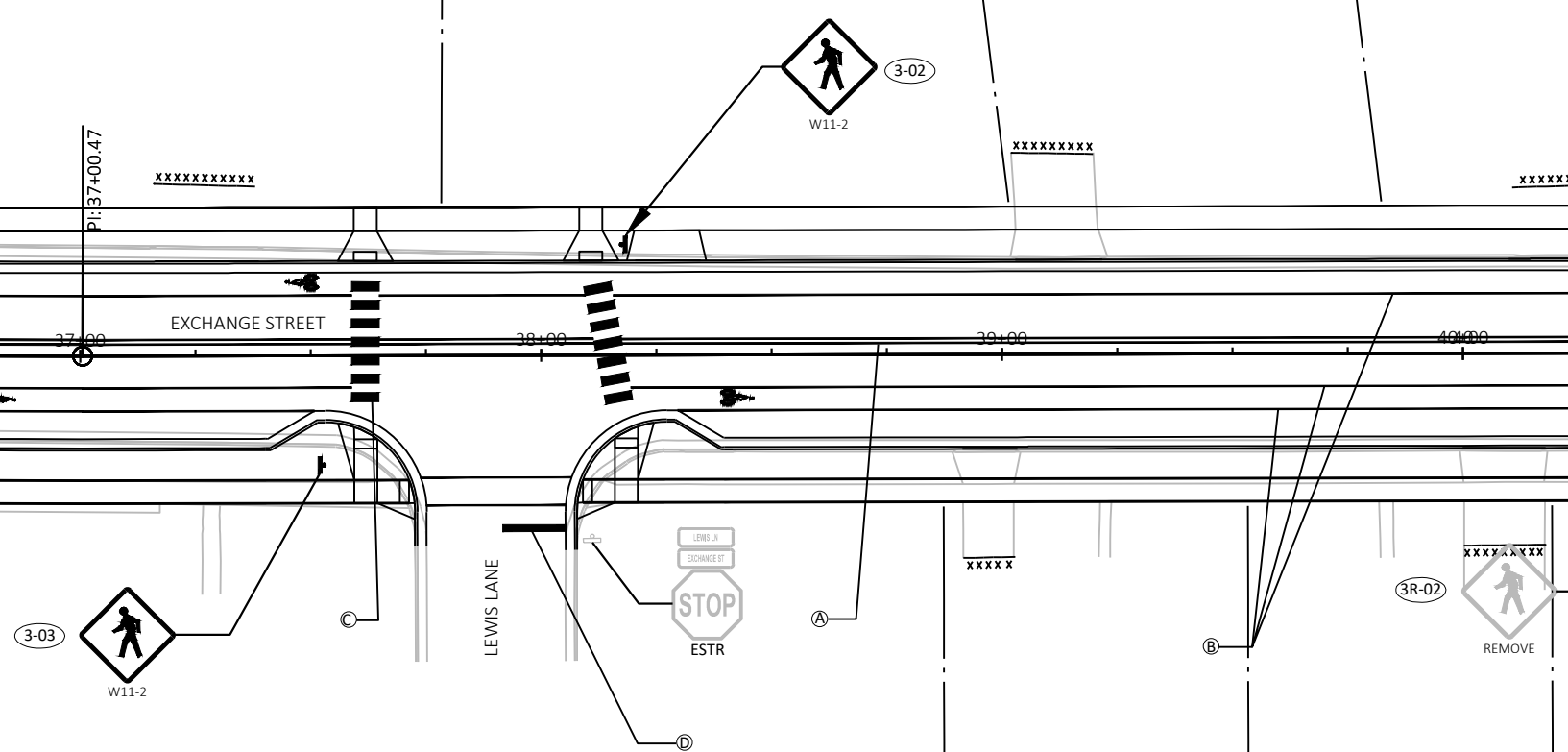
MATCHLINE AT STA. 34+93

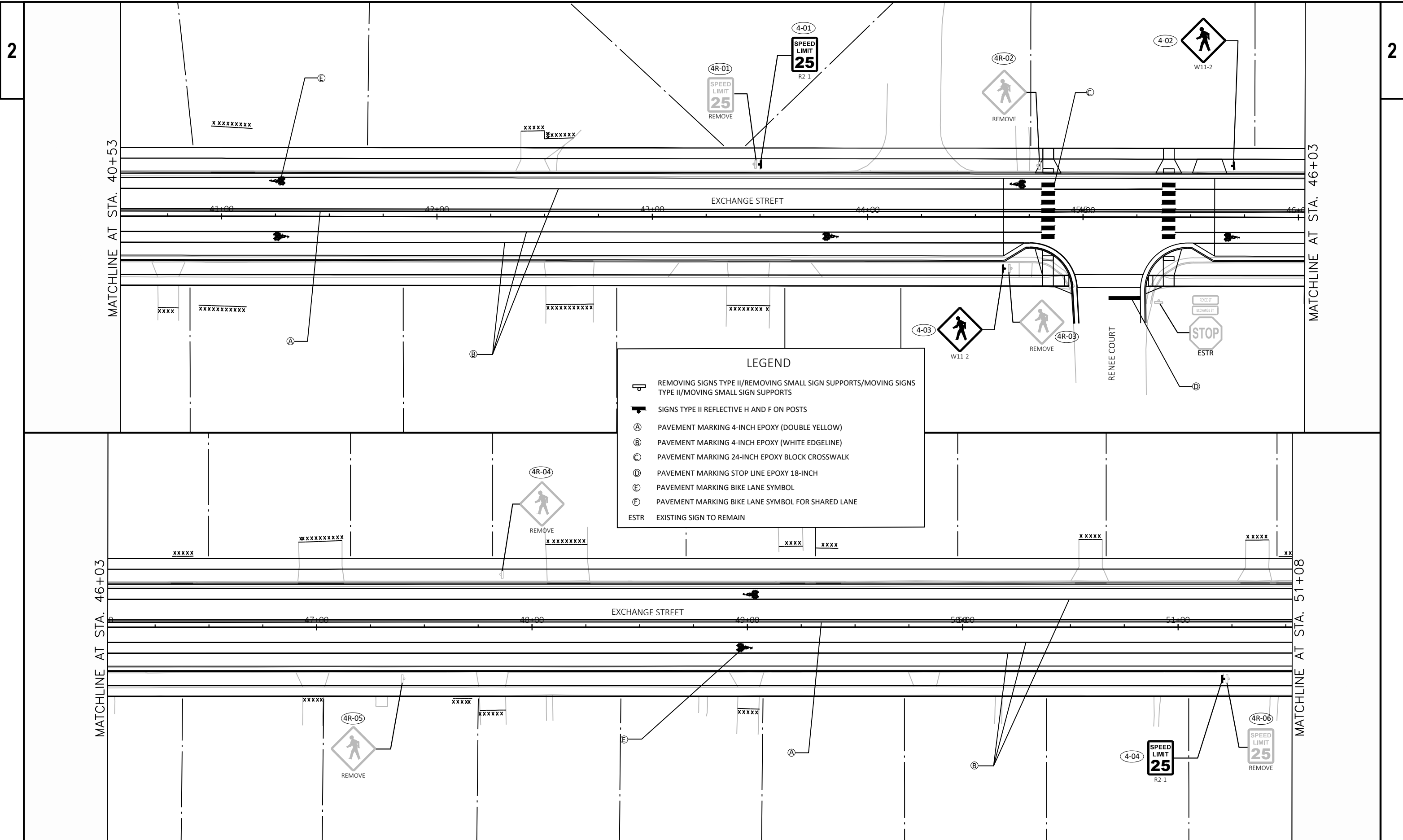


LEGEND	
	REMOVING SIGNS TYPE II/REMOVING SMALL SIGN SUPPORTS/MOVING SIGNS TYPE II/MOVING SMALL SIGN SUPPORTS
	SIGNS TYPE II REFLECTIVE H AND F ON POSTS
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	PAVEMENT MARKING 4-INCH EPOXY (WHITE EDGELINE)
	PAVEMENT MARKING 24-INCH EPOXY BLOCK CROSSWALK
	PAVEMENT MARKING STOP LINE EPOXY 18-INCH
	PAVEMENT MARKING BIKE LANE SYMBOL
	PAVEMENT MARKING BIKE LANE SYMBOL FOR SHARED LANE
ESTR	EXISTING SIGN TO REMAIN

MATCHLINE AT STA. 34+93

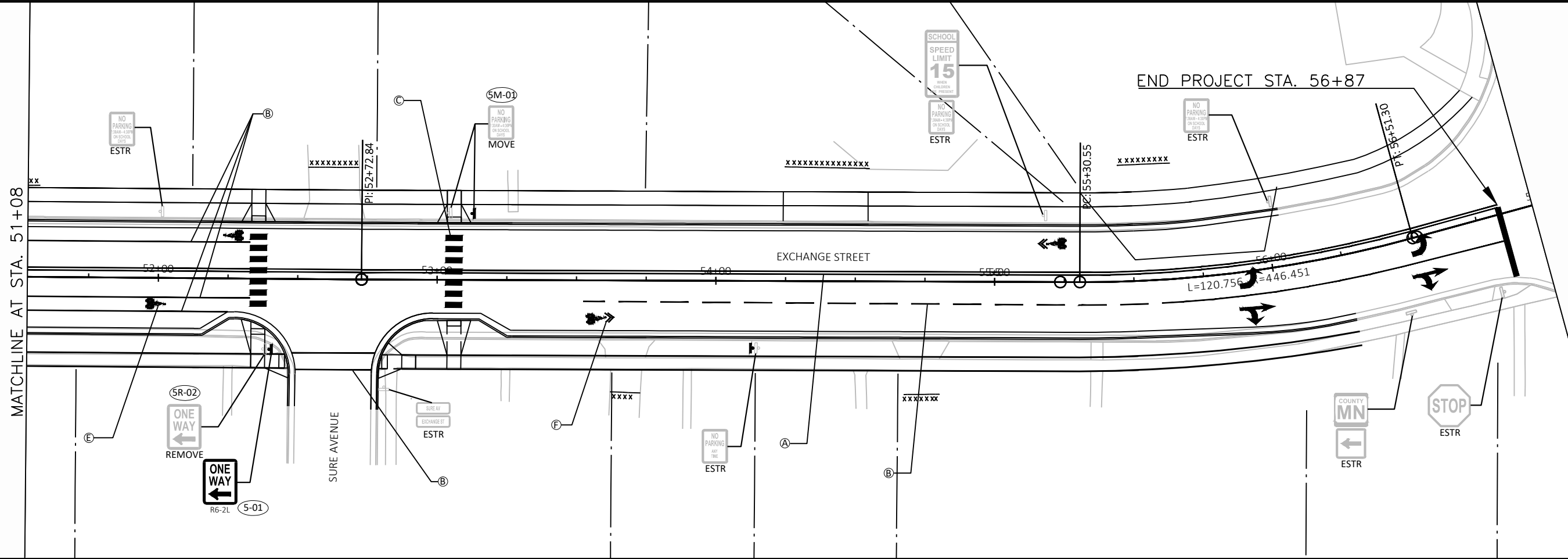
MATCHLINE AT STA. 40+53













LEGEND

	REMOVING SIGNS TYPE II/REMOVING SMALL SIGN SUPPORTS/MOVING SIGNS TYPE II/MOVING SMALL SIGN SUPPORTS
	SIGNS TYPE II REFLECTIVE H AND F ON POSTS
	PAVEMENT MARKING 4-INCH EPOXY (DOUBLE YELLOW)
	PAVEMENT MARKING 4-INCH EPOXY (WHITE EDGELINE)
	PAVEMENT MARKING 24-INCH EPOXY BLOCK CROSSWALK
	PAVEMENT MARKING STOP LINE EPOXY 18-INCH
	PAVEMENT MARKING BIKE LANE SYMBOL
	PAVEMENT MARKING BIKE LANE SYMBOL FOR SHARED LANE
	EXISTING SIGN TO REMAIN



LEGEND

-  REMOVING SIGNS TYPE II/REMOVING SMALL SIGN SUPPORTS/MOVING SIGNS TYPE II/MOVING SMALL SIGN SUPPORTS
-  SIGNS TYPE II REFLECTIVE H AND F ON POSTS
-  PAVEMENT MARKING 4-INCH EPOXY (DOUBLE YELLOW)
-  PAVEMENT MARKING 4-INCH EPOXY (WHITE EDGELINE)
-  PAVEMENT MARKING 24-INCH EPOXY BLOCK CROSSWALK
-  PAVEMENT MARKING STOP LINE EPOXY 18-INCH
-  PAVEMENT MARKING BIKE LANE SYMBOL
-  PAVEMENT MARKING BIKE LANE SYMBOL FOR SHARED LANE
- ESTR EXISTING SIGN TO REMAIN

Estimate Of Quantities

5685-00-05 5685-00-06

Line	Item	Item Description	Unit	Total	Qty	Qty
0002	201.0120	Clearing	ID	555.000	555.000	
0004	201.0220	Grubbing	ID	555.000	555.000	
0006	204.0110	Removing Asphaltic Surface	SY	18,700.000	18,700.000	
0008	204.0150	Removing Curb & Gutter	LF	8,560.000	8,560.000	
0010	204.0155	Removing Concrete Sidewalk	SY	2,060.000	2,060.000	
0012	204.0220	Removing Inlets	EACH	9.000	9.000	
0014	204.0245	Removing Storm Sewer (size) 01. 12-inch	LF	200.000	200.000	
0016	204.0245	Removing Storm Sewer (size) 02. 15-inch	LF	117.000	117.000	
0018	205.0100	Excavation Common	CY	15,520.000	15,520.000	
0020	213.0100	Finishing Roadway (project) 01. 5685-00-05	EACH	1.000	1.000	
0022	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	8,785.000	8,785.000	
0024	305.0130	Base Aggregate Dense 3-Inch	TON	14,410.000	14,410.000	
0026	312.0110	Select Crushed Material	TON	2,610.000	2,610.000	
0028	455.0605	Tack Coat	GAL	540.000	540.000	
0030	460.2000	Incentive Density HMA Pavement	DOL	3,360.000	3,360.000	
0032	460.5224	HMA Pavement 4 LT 58-28 S	TON	3,500.000	3,500.000	
0034	460.5225	HMA Pavement 5 LT 58-28 S	TON	1,750.000	1,750.000	
0036	465.0120	Asphaltic Surface Driveways and Field Entrances	TON	60.000	60.000	
0038	601.0411	Concrete Curb & Gutter 30-Inch Type D	LF	8,168.000	8,168.000	
0040	601.0600	Concrete Curb Pedestrian	LF	110.000	110.000	
0042	602.0405	Concrete Sidewalk 4-Inch	SF	33,200.000	33,200.000	
0044	602.0415	Concrete Sidewalk 6-Inch	SF	6,315.000	6,315.000	
0046	602.0515	Curb Ramp Detectable Warning Field Natural Patina	SF	272.000	272.000	
0048	602.0810	Concrete Driveway 6-Inch	SY	1,350.000	1,350.000	
0050	608.0312	Storm Sewer Pipe Reinforced Concrete Class III 12-Inch	LF	1,547.000	1,547.000	
0052	608.0318	Storm Sewer Pipe Reinforced Concrete Class III 18-Inch	LF	774.000	774.000	
0054	611.0530	Manhole Covers Type J	EACH	6.000	6.000	
0056	611.0624	Inlet Covers Type H	EACH	26.000	26.000	
0058	611.2004	Manholes 4-FT Diameter	EACH	6.000	6.000	
0060	611.3230	Inlets 2x3-FT	EACH	26.000	26.000	
0062	611.8110	Adjusting Manhole Covers	EACH	21.000	21.000	
0064	619.1000	Mobilization	EACH	1.000	1.000	
0066	624.0100	Water	MGAL	200.000	200.000	
0068	625.0100	Topsoil	SY	9,350.000	9,350.000	
0070	627.0200	Mulching	SY	9,350.000	9,350.000	
0072	628.1905	Mobilizations Erosion Control	EACH	2.000	2.000	
0074	628.1910	Mobilizations Emergency Erosion Control	EACH	2.000	2.000	
0076	629.0210	Fertilizer Type B	CWT	6.000	6.000	
0078	630.0140	Seeding Mixture No. 40	LB	168.000	168.000	
0080	630.0500	Seed Water	MGAL	168.000	168.000	
0082	634.0810	Posts Tubular Steel 2x2-Inch X 10-FT	EACH	18.000	18.000	
0084	637.2210	Signs Type II Reflective H	SF	24.250	24.250	
0086	637.2230	Signs Type II Reflective F	SF	78.500	78.500	
0088	638.2102	Moving Signs Type II	EACH	2.000	2.000	
0090	638.2602	Removing Signs Type II	EACH	16.000	16.000	
0092	638.3000	Removing Small Sign Supports	EACH	16.000	16.000	
0094	638.4000	Moving Small Sign Supports	EACH	2.000	2.000	
0096	642.5001	Field Office Type B	EACH	1.000	1.000	
0098	643.0300	Traffic Control Drums	DAY	52,049.000	52,049.000	
0100	643.0410	Traffic Control Barricades Type II	DAY	1,284.000	1,284.000	

Estimate Of Quantities

5685-00-05 5685-00-06

Line	Item	Item Description	Unit	Total	Qty	Qty
0102	643.0420	Traffic Control Barricades Type III	DAY	3,745.000	3,745.000	
0104	643.0705	Traffic Control Warning Lights Type A	DAY	3,745.000	3,745.000	
0106	643.0900	Traffic Control Signs	DAY	3,103.000	3,103.000	
0108	643.0920	Traffic Control Covering Signs Type II	EACH	5.000	5.000	
0110	643.3970	Temporary Marking Removable Mask Out Tape 10-Inch	LF	40.000	40.000	
0112	643.5000	Traffic Control	EACH	1.000	1.000	
0114	644.1440	Temporary Pedestrian Surface Matting	SF	7,700.000	7,700.000	
0116	644.1601	Temporary Pedestrian Curb Ramp	DAY	24.000	24.000	
0118	644.1605	Temporary Pedestrian Detectable Warning Field	SF	440.000	440.000	
0120	644.1810	Temporary Pedestrian Barricade	LF	1,424.000	1,424.000	
0122	646.1020	Marking Line Epoxy 4-Inch	LF	20,323.000	20,323.000	
0124	646.5020	Marking Arrow Epoxy	EACH	4.000	4.000	
0126	646.5220	Marking Symbol Epoxy	EACH	33.000	33.000	
0128	646.6120	Marking Stop Line Epoxy 18-Inch	LF	115.000	115.000	
0130	646.7420	Marking Crosswalk Epoxy Transverse Line 6-Inch	LF	325.000	325.000	
0132	646.7520	Marking Crosswalk Epoxy Block Style 24-Inch	LF	462.000	462.000	
0134	650.4000	Construction Staking Storm Sewer	EACH	32.000	32.000	
0136	650.4500	Construction Staking Subgrade	LF	4,322.000	4,322.000	
0138	650.5000	Construction Staking Base	LF	4,322.000	4,322.000	
0140	650.5500	Construction Staking Curb Gutter and Curb & Gutter	LF	8,278.000	8,278.000	
0142	650.9000	Construction Staking Curb Ramps	EACH	34.000	34.000	
0144	650.9500	Construction Staking Sidewalk (project) 01. 5685-00-05	EACH	1.000	1.000	
0146	650.9911	Construction Staking Supplemental Control (project) 01. 5685-00-05	EACH	1.000	1.000	
0148	650.9920	Construction Staking Slope Stakes	LF	4,322.000	4,322.000	
0150	652.0225	Conduit Rigid Nonmetallic Schedule 40 2-Inch	LF	2,540.000	2,540.000	
0152	653.0154	Pull Boxes Non-Conductive 24x36-Inch	EACH	4.000	4.000	
0154	655.0625	Electrical Wire Lighting 6 AWG	LF	7,620.000	7,620.000	
0156	659.2124	Lighting Control Cabinets 120/240 24-Inch	EACH	1.000	1.000	
0158	690.0150	Sawing Asphalt	LF	215.000	215.000	
0160	740.0440	Incentive IRI Ride	DOL	3,292.000	3,292.000	
0162	ASP.1T0A	On-the-Job Training Apprentice at \$5.00/HR	HRS	450.000	450.000	
0164	ASP.1T0G	On-the-Job Training Graduate at \$5.00/HR	HRS	400.000	400.000	
0166	SPV.0060	Special 01. Inlet Protection Type D-RF	EACH	43.000	43.000	
0168	SPV.0060	Special 02. Remove and Reinstall Mailbox	EACH	59.000	59.000	
0170	SPV.0060	Special 03. Existing Sanitary Sewer Connection	EACH	8.000		8.000
0172	SPV.0060	Special 04. Sanitary Lateral Tracer Wire Box	EACH	15.000		15.000
0174	SPV.0060	Special 05. Sanitary Service Lateral Connection	EACH	16.000		16.000
0176	SPV.0060	Special 06. Sanitary Sewer 18"x4" Wye	EACH	14.000		14.000
0178	SPV.0060	Special 07. Sanitary Sewer 8"x4" Wye	EACH	2.000		2.000
0180	SPV.0060	Special 08. Sanitary Manhole Casting	EACH	15.000		15.000
0182	SPV.0060	Special 09. 10" Valves and Boxes	EACH	17.000		17.000
0184	SPV.0060	Special 10. 8" Valves and Boxes	EACH	2.000		2.000
0186	SPV.0060	Special 11. 6" Valves and Boxes	EACH	6.000		6.000
0188	SPV.0060	Special 12. 1" Curb Stops	EACH	49.000		49.000
0190	SPV.0060	Special 13. 1" Corporation Stops	EACH	49.000		49.000
0192	SPV.0060	Special 14. Water Service Lateral Reconnections	EACH	49.000		49.000
0194	SPV.0060	Special 15. Existing Main Reconnections	EACH	13.000		13.000
0196	SPV.0060	Special 16. New Hydrants	EACH	9.000		9.000
0198	SPV.0060	Special 17. Existing Hydrant Removals	EACH	9.000		9.000
0200	SPV.0060	Special 18. Lighting Unit Type Special 1	EACH	11.000	11.000	

Estimate Of Quantities

5685-00-05 5685-00-06

Line	Item	Item Description	Unit	Total	Qty	Qty
0202	SPV.0060	Special 19. Lighting Unit Type Special 2	EACH	7.000	7.000	
0204	SPV.0060	Special 20. Utility Line Opening	EACH	10.000		10.000
0206	SPV.0060	Special 21. Sanitary Sewer Spot Repair	EACH	2.000		2.000
0208	SPV.0090	Special 01. Sanitary Sewer, PVC 18-Inch	LF	775.000		775.000
0210	SPV.0090	Special 02. Sanitary Sewer, PVC 12-Inch	LF	14.000		14.000
0212	SPV.0090	Special 03. Sanitary Sewer, PVC 8-Inch	LF	25.000		25.000
0214	SPV.0090	Special 04. Sanitary Sewer, PVC 4-Inch	LF	616.000		616.000
0216	SPV.0090	Special 05. 12" Water Main Ductile Iron	LF	22.000		22.000
0218	SPV.0090	Special 06. 10" Water Main, Ductile Iron	LF	2,535.000		2,535.000
0220	SPV.0090	Special 07. 8" Water Main, Ductile Iron	LF	60.000		60.000
0222	SPV.0090	Special 08. 6" Water Main, Ductile Iron	LF	200.000		200.000
0224	SPV.0090	Special 09. 1" Copper Water Service Laterals	LF	1,372.000		1,372.000
0226	SPV.0090	Special 10. Temporary Sewer Bypassing and Connections	LF	729.000		729.000
0228	SPV.0200	Special 01. Sanitary Manhole Masonry 48-Inch Diameter	VF	40.200		40.200

CLEARING AND GRUBBING

STATION	-	STATION	LOCATION	201.0120 ID	201.0220 ID
12+51	-	55+73	EXCHANGE	555	555

REMOVING ASPHALTIC SURFACE

STATION - STATION	LOCATION	204.0110 SY
12+73.00 - 53+73.00	EXCHANGE	18700
TOTAL		18700

REMOVING CURB & GUTTER

STATION - STATION	LOCATION	204.0150 LF
13+23.20 - 17+70.35	LT	451
17+86.73 - 55+73.12	LT	3797
12+42.15 - 21+56.60	RT	931
21+82.87 - 24+37.56	RT	289
24+75.23 - 37+75.02	RT	1325
38+05.17 - 44+96.14	RT	709
45+29.47 - 52+49.26	RT	746
52+76.39 - 55+73.13	RT	312
TOTAL		8560

REMOVING CONCRETE SIDEWALK

STATION - STATION	LOCATION	204.0155 SY
12+42.15 - 21+56.60	RT	440
21+82.87 - 24+37.56	RT	150
24+75.23 - 37+75.02	RT	710
38+05.17 - 44+96.14	RT	300
45+29.47 - 52+49.26	RT	320
52+76.39 - 55+73.13	RT	140
TOTAL		2060

STORM SEWER REMOVALS

STATION - STATION	LOCATION	REMOVING INLETS EACH	204.0245.01	204.0245.02
			REMOVING STORM SEWER 12-INCH LF	REMOVING STORM SEWER 15-INCH LF
15+28.03	22.31' RT	1	---	---
15+74.04	19.06' LT	1	---	---
18+07.07	19.57' RT	1	---	---
18+15.93	19.50' RT	1	---	---
24+31.30	22.02' LT	1	---	---
24+35.31	32.72' RT	1	---	---
24+88.13	23.46' RT	1	---	---
24+91.52	21.98' RT	1	---	---
52+32.37	20.01' RT	1	---	---
15+28.03 - 15+37.80	RT	---	10	---
15+37.80 - 15+74.04	RT/LT	---	54	---
17+72.11 - 18+15.93	RT/LT	---	---	80
18+07.07 - 18+15.93	RT	---	8	---
18+15.93 - 18+53.82	RT	---	---	37
24+31.30 - 24+35.31	RT/LT	---	54	---
24+35.31 - 24+91.52	RT/LT	---	53	---
52+32.37 - 52+47.97	RT	---	21	---
TOTAL		9	200	117

EXCAVATION COMMON

STATION	-	STATION	205.0100	312.0110	UNEXPANDED FILL CY	MASS ORDINATE +/- CY
			COMMON EXCAVATION (1) CUT (2) CY	SELECT EBS EXCAVATION (3) CY		
12+51	-	55+73	14110	1410	0	12930
SUBTOTAL			14110	1410	0	12930
TOTALS			15520	2610	0	0

Notes:

- 1) Common Excavation is the sum of the Cut and EBS Excavation columns. Item number 205.0100
- 2) Cut volume includes concrete and asphaltic surface material
- 3) All EBS material is to be wasted offsite. EBS quantity estimated using 10.0% of the cut volume.
- 4) The Mass Ordinate + or - Qty calculated for the Division. Negative indicates a shortage of material within
- 5) EBS Excavation to be backfilled with Select Crushed Material.

BASE AGGREGATE DENSE

		305.0120 1 1/4-INCH		305.0130 3 INCH		624.0100 WATER	
		CATEGORY 0010	CATEGORY 0020	CATEGORY 0010	CATEGORY 0020	CATEGORY 0010	CATEGORY 0020
STATION -	STATION	TON	TON	TON	TON	MGAL	MGAL
12+51 -	55+73	7730	1040	12700	1710	180	20
DRIVEWAYS		15					0
SUBTOTAL		7745	1040	12700	1710	180	20
TOTAL		8785		14410		200	

HMA PAVEMENT

		455.0605 TACK COAT		460.5224 HMA PAVEMENT 4 LT 58-28 S		460.5225 HMA PAVEMENT 5 LT 58-28 S	
		CATEGORY 00	CATEGORY 0020	CATEGORY 0010	CATEGORY 0020	CATEGORY 0010	CATEGORY 0020
STATION -	STATION	GAL	GAL	TON	TON	TON	TON
12+51 -	55+73	470	70	3080	420	1540	210
SUBTOTAL		470	70	3080	420	1540	210
TOTAL		540		3500		1750	

CONCRETE CURB & GUTTER

		601.0411 CONCRETE CURB & GUTTER 30-INCH TYPE D		601.0600 CONCRETE CURB PEDESTRIAN		650.5500 CONSTRUCTION STAKING CURB GUTTER AND CURB & GUTTER	
STATION -	STATION	LOCATION	LF	LF	LF	LF	LF
12+76.70 -	17+67.02	LT	496	-	496		
17+83.80 -	55+73.13	LT	3795	110	3905		
12+42.15 -	21+56.60	RT	943	-	943		
21+82.89 -	24+37.58	RT	302	-	302		
24+75.25 -	37+75.05	RT	1341	-	1341		
38+05.17 -	44+97.87	RT	193	-	193		
45+26.84 -	52+49.26	RT	779	-	779		
52+76.39 -	55+73.13	RT	319	-	319		
TOTAL			8168	110	8278		

CONCRETE SIDEWALK

		602.0405 CONCRETE SIDEWALK 4-INCH		602.0415 CONCRETE SIDEWALK 6-INCH		650.9500 CONSTRUCTION STAKING SIDEWALK (PROJECT)	
STATION -	STATION	LOCATION	SF	SF	EACH	EACH	EACH
17+83.80 -	55+73.13	LT	15750	3350	-	-	-
12+42.15 -	21+56.60	RT	3700	560	-	-	-
21+82.89 -	24+37.58	RT	1250	70	-	-	-
24+75.25 -	37+75.05	RT	5520	900	-	-	-
38+05.17 -	44+97.87	RT	2550	760	-	-	-
45+26.84 -	52+49.26	RT	3020	450	-	-	-
52+76.39 -	56+72.07	RT	1410	225	-	-	-
UNDISTRIBUTED		-	-	-	1	-	-
TOTAL			33200	6315	1		

ADJUSTING MANHOLE COVERS

		611.8110	
STATION	LOCATION	EACH	EACH
38+53.53	1' RT	1	1
42+01.33	1' RT	1	1
45+69.17	2' RT	1	1
48+72.20	1' RT	1	1
54+64.47	2' LT	1	1
13+67.48	22' RT	1	1
17+65.62	8' LT	1	1
19+66.63	10' LT	1	1
21+73.69	12' LT	1	1
22+65.42	2' LT	1	1
25+01.80	1' LT	1	1
26+43.42	2' LT	1	1
28+98.35	3' LT	1	1
33+01.29	4' LT	1	1
36+40.94	12' LT	1	1
38+77.12	14' LT	1	1
41+32.09	12' LT	1	1
45+11.30	18' LT	1	1
48+62.38	16' LT	1	1
52+54.40	15' LT	1	1
54+63.84	14' LT	1	1
TOTAL		21	21

ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES

		465.0120	
STATION -	STATION	LOCATION	TON
17+86.73 -	55+73.12	LT	53
21+82.87 -	24+37.56	RT	0
24+75.23 -	37+75.02	RT	0
38+05.17 -	44+96.14	RT	3
45+29.47 -	52+49.26	RT	2
52+76.39 -	55+73.13	RT	2
TOTAL			60

CONCRETE DRIVEWAY 6-INCH

		602.0810	
STATION -	STATION	LOCATION	SY
13+23.20 -	17+70.35	LT	45
17+86.73 -	55+73.12	LT	495
12+42.15 -	21+56.60	RT	215
21+82.87 -	24+37.56	RT	25
24+75.23 -	37+75.02	RT	230
38+05.17 -	44+96.14	RT	200
45+29.47 -	52+49.26	RT	90
52+76.39 -	56+72.07	RT	50
TOTAL			1350

STORM SEWER - STRUCTURES

STATION	DUCT	611.0530 MANHOLE COVERS TYPE J EACH	611.2004 MANHOLES 4-FT DIAMETER EACH	611.3230 INLETS 2X3-FT EACH	611.0624 INLET COVERS TYPE H EACH	650.4000 CONSTRUCTION STAKING STORM SEWER EACH	TOP OF STRUCTURE ELEVATION	INVERT ELEVATION	DEPTH
15+21.32	73	---	---	1	1	1	851.23	848.72	2.51
15+31.80	74	---	---	1	1	1	851.89	848.75	3.14
15+39.64	67	---	---	1	1	1	851.85	848.71	3.14
15+74.32	68	---	---	1	1	1	851.82	848.45	3.37
16+58.52	71	---	---	1	1	1	852.44	848.88	3.56
16+81.56	72	---	---	1	1	1	852.47	849.10	3.37
18+05.09	79	---	---	1	1	1	853.62	847.12	6.5
18+40.13	78	---	---	1	1	1	853.34	847.48	5.86
18+53.82	83	1	1	---	---	1	854.58	848.90	5.68
21+21.61	82	---	---	1	1	1	854.92	851.51	3.41
22+19.35	80	---	---	1	1	1	854.46	851.54	2.92
22+25.52	81	---	---	1	1	1	854.48	851.58	2.90
23+98.65	85	---	---	1	1	1	853.93	850.40	3.53
24+03.14	65	---	---	1	1	1	853.94	849.35	4.59
24+31.20	66	---	---	1	1	1	852.32	849.10	3.22
24+36.30	86	---	---	1	1	1	854.21	850.57	3.64
25+12.24	64	---	---	1	1	1	854.37	851.28	3.09
38+42.74	57	---	---	1	1	1	889.30	885.33	3.97
38+45.28	58	---	---	1	1	1	888.56	884.60	3.96
38+53.53	56	1	1	---	---	1	889.30	884.39	4.91
41+93.36	54	---	---	1	1	1	885.93	881.96	3.97
42+00.16	53	---	---	1	1	1	885.74	881.77	3.97
42+01.33	50	1	1	---	---	1	885.94	881.48	4.46
44+59.59	60	---	---	1	1	1	881.63	877.66	3.97
45+56.29	55	---	---	1	1	1	879.86	875.90	3.96
45+64.14	52	---	---	1	1	1	880.48	876.42	4.06
45+69.17	51	1	1	---	---	1	880.49	875.25	5.24
48+72.20	94	1	1	---	---	1	878.45	871.66	6.79
52+11.14	61	---	---	1	1	1	873.34	869.38	3.96
52+75.46	103	---	---	1	1	1	870.66	866.94	3.72
52+47.97	107	---	---	1	1	1	873.28	867.73	5.55
52+61.87	108	1	1	---	---	1	871.21	866.53	4.68
TOTAL		6	6	26	26	32			

STORM SEWER - PIPE

STRUCTURE	TO	STRUCTURE	608.0312 STORM SEWER PIPE REINFORCED CONCRETE CLASS III	608.0318 STORM SEWER PIPE REINFORCED CONCRETE CLASS III
			12-INCH LF	18-INCH LF
73	-	68	51	---
68	-	71	83	---
71	-	72	45	---
68	-	67	52	---
74	-	67	8	---
EX 33	-	79	44	---
79	-	78	52	---
78	-	83	20	---
82	-	EX 30	39	---
EX 29	-	80	39	---
80	-	81	4	---
66	-	65	---	48
85	-	65	2	---
65	-	86	---	36
86	-	64	76	---
58	-	56	20	---
56	-	57	20	---
56	-	50	345	---
54	-	50	20	---
50	-	53	20	---
50	-	51	365	---
55	-	51	20	---
51	-	52	20	---
60	-	52	102	---
51	-	94	---	300
94	-	108	---	390
61	-	107	38	---
107	-	108	38	---
108	-	103	24	---
TOTAL			1547	774

CONSTRUCTION STAKING

STATION - STATION	LOCATION	650.4500 SUBGRADE LF	650.5000 BASE LF	650.9911 SUPPLEMENTAL CONTROL (PROJECT) EACH	650.9920 SLOPE STAKES LF
12+51 - 55+73	EXCHANGE	4322	4322	1	4322
TOTAL		4322	4322	1	4322

SAWING ASPHALT

STATION	LOCATION	690.0150 LF
12+70	EXCHANGE ST	30
17+67	- 17+86 LT	20
21+57	- 21+83 RT	25
24+37	- 24+75 RT	40
37+75	- 38+05 RT	30
44+98	- 45+27 RT	30
55+73	EXCHANGE ST	40
TOTAL		215

CURB RAMPS

STATION	LOCATION	602.0515	650.9000
		CURB RAMP DETECTABLE WARNING FIELD NATURAL PATINA	CONSTRUCTION STAKING CURB RAMPS
		SF	EACH
12+54.86	RT	8	1
12+72.78	RT	8	1
17+78.27	LT	8	1
18+13.06	LT	8	1
18+13.59	RT	8	1
21+24.56	RT	8	1
21+24.56	LT	8	1
21+53.41	RT	8	1
21+88.21	RT	8	1
21+96.66	LT	8	1
21+98.89	RT	8	1
24+24.23	RT	8	1
24+24.63	LT	8	1
24+32.95	RT	8	1
24+80.16	RT	8	1
24+80.55	LT	8	1
24+90.08	RT	8	1
37+61.60	RT	8	1
37+62.03	LT	8	1
37+70.40	RT	8	1
38+10.39	RT	8	1
38+10.61	LT	8	1
38+19.11	RT	8	1
44+81.33	RT	8	1
44+83.84	LT	8	1
44+92.35	RT	8	1
45+31.93	RT	8	1
45+39.68	LT	8	1
45+40.70	RT	8	1
52+33.43	RT	8	1
52+35.72	LT	8	1
52+44.38	RT	8	1
52+81.76	RT	8	1
52+89.89	LT	8	1
52+91.91	RT	8	1
TOTAL		272	34

SIGNING MOVING AND REMOVALS

SIGN #	SIGN CODE	638.2602	638.3000	638.2102	638.4000
		REMOVING SIGNS TYPE II EACH	REMOVING SMALL SIGN SUPPORTS EACH	MOVING SIGNS TYPE II EACH	MOVING SMALL SIGN SUPPORTS EACH
1M-01	R1-1	-	-	1	1
2R-01	S4-51	1	1	-	-
2R-02	R2-1	1	1	-	-
2R-03	R1-1	1	1	-	-
2R-04	W11-2	1	1	-	-
2R-05	S4-51	1	1	-	-
2R-06	W11-2	1	1	-	-
2R-07	W11-2	1	1	-	-
3R-01	R2-1C	1	1	-	-
3R-02	W11-2	1	1	-	-
4R-01	R2-1	1	1	-	-
4R-02	W11-2	1	1	-	-
4R-03	W11-2	1	1	-	-
4R-04	W11-2	1	1	-	-
4R-05	W11-2	1	1	-	-
4R-06	R2-1	1	1	-	-
5M-01	CUSTOM	0	0	1	1
5R-02	R6-2L	1	1	-	-
TOTAL		16	16	2	2

FINISHING ITEMS

STATION - STATION	LOCATION	625.0100	627.0200	629.0210	630.0140	630.0500
		TOPSOIL SY	MULCHING SY	FERTILIZER TYPE B CWT	SEEDING MIXTURE NO. 40 LB	SEED WATER MGAL
13+23.20 - 17+70.35	LT	500	500	0.3	9.0	9
17+86.73 - 55+73.12	LT	3160	3160	2.0	57.0	57
12+42.15 - 21+56.60	RT	830	830	0.5	15.0	15
21+82.87 - 24+37.56	RT	300	300	0.2	5.5	5.5
24+75.23 - 37+75.02	RT	1180	1180	0.8	21.0	21
38+05.17 - 44+96.14	RT	610	610	0.4	11.0	11
45+29.47 - 52+49.26	RT	640	640	0.4	11.5	11.5
52+76.39 - 55+73.13	RT	260	260	0.2	4.5	4.5
UNDISTRIBUTED		1870	1870	1.3	33.5	33.5
TOTAL		9350	9350	6	168	168

**REMOVE AND REINSTALL MAILBOX
CATEGORY 0020**

STATION	-	STATION	SPV.0060.02 EACH
12+73	-	53+73	59

STREET LIGHTING - CATEGORY 0020

STATION	EACH	653.0154	652.0225	655.0625	659.2124	SPV.0060.18	SPV.0060.19
		PULL BOXES NON- CONDUCTIVE 24X36-INCH	CONDUIT RIGID NONMETALLIC SCHEDULE 40 (2")	ELECTRIC WIRE LIGHTING 6 AWG	LIGHTING CONTROL CABINET 120/240 24-IN	LIGHTING UNIT TYPE SPECIAL 1	LIGHTING UNIT TYPE SPECIAL 2
		L.F.	L.F.	EACH	EACH	EACH	
12+51 - 55+73	4	2540	7620	1	11	7	
TOTAL	4	2540	7620	1	11	7	

PERMANENT SIGNING

SIGN #	SIGN CODE	SIGN SIZE	SIGN SIZE WXH IN	SIGNS TYPE II		POSTS TUBULAR STEEL 2X2-INCH X 10-FT EACH	REMARKS
				637.2210 REFLECTIVE H	637.2230 REFLECTIVE F		
				SF	SF		
1-01	W11-2	2S	30X30	-	6.25	1	PEDESTRIAN CROSSING SYMBOL
1-02	W11-2	2S	30X30	-	6.25	1	PEDESTRIAN CROSSING SYMBOL
2-01	W11-2	2S	30X30	-	6.25	1	PEDESTRIAN CROSSING SYMBOL
2-02	W11-2	2S	30X30	-	6.25	1	PEDESTRIAN CROSSING SYMBOL
2-03	S4-51	2S	24X48	-	8.00	1	SCHOOL SPEED LIMIT 15 MPH
2-04	R2-1	2S	24X30	5.00	-	1	SPEED LIMIT 25 MPH
2-05	W11-2	2S	30X30	-	6.25	1	PEDESTRIAN CROSSING SYMBOL
2-06	R1-1	2S	30X30	6.25	-	1	STOP
2-07	S4-51	2S	24X48	-	8.00	1	SCHOOL SPEED LIMIT 15 MPH
2-08	W11-2	2S	30X30	-	6.25	1	PEDESTRIAN CROSSING SYMBOL
3-01	R2-1C	-	-	X	X	1	YOUR SPEED (DYNAMIC MESSAGE SPEED SIGN)
3-02	W11-2	2S	30X30	-	6.25	1	PEDESTRIAN CROSSING SYMBOL
3-03	W11-2	2S	30X30	-	6.25	1	PEDESTRIAN CROSSING SYMBOL
4-01	R2-1	2S	24X30	5.00	-	1	SPEED LIMIT 25 MPH
4-02	W11-2	2S	30X30	-	6.25	1	PEDESTRIAN CROSSING SYMBOL
4-03	W11-2	2S	30X30	-	6.25	1	PEDESTRIAN CROSSING SYMBOL
4-04	R2-1	2S	24X30	5.00	-	1	SPEED LIMIT 25 MPH
5-01	R6-2L	1	18X24	3.00	-	1	ONE WAY LEFT ARROW
TOTAL				24.25	78.50	18.00	

INLET PROTECTION TYPE D-RF

STATION	STRUCTURE	EACH
15+21.32	73	1
15+31.80	74	1
15+39.64	67	1
15+74.32	68	1
16+58.52	71	1
16+81.56	72	1
18+05.09	79	1
18+40.13	78	1
21+21.61	82	1
22+19.35	80	1
22+25.52	81	1
23+98.65	85	1
24+03.14	65	1
24+31.20	66	1
24+36.30	86	1
25+12.24	64	1
38+42.74	57	1
38+45.28	58	1
41+93.36	54	1
42+00.16	53	1
44+59.59	60	1
45+56.29	55	1
45+64.14	52	1
52+11.14	61	1
52+75.46	103	1
52+47.97	107	1
15+28.03	89	1
15+38.09	90	1
15+74.23	91	1
18+07.19	35	1
18+16.05	32	1
20+03.91	31	1
21+54.35	30	1
21+53.97	36	1
21+85.15	29	1
24+31.37	27	1
24+34.25	26	1
24+88.67	25	1
24+92.38	24	1
52+46.49	38	1
52+75.47	49	1
56+52.02	43	1
56+81.26	42	1
TOTAL		43

TRAFFIC CONTROL

STATION	643.0300	643.0410	643.0420	643.0705	643.0900	643.5000
	TRAFFIC CONTROL DRUMS DAYS	TRAFFIC CONTROL BARRICADES TYPE II DAYS	TRAFFIC CONTROL BARRICADES TYPE III DAYS	TRAFFIC CONTROL LIGHTS TYPE A DAYS	TRAFFIC CONTROL SIGNS DAYS	TRAFFIC CONTROL EACH
12+51 - 55+73	486*	12*	35*	35*	29*	1*
ENDAR DAYS / UNIT	107*	107*	107*	107*	107*	
TOTAL	52049	1284	3745	3745	3103	1

TRAFFIC CONTROL - CONTINUED

STATION	STATION	LOCATION	643.0920 COVERING SIGNS TYPE II	643.3970 TEMPORARY MARKING REMOVABLE MASK OUT TAPE 10-INCH
			EACH	LF
N/A		JOHNSON ST	5	40
TOTALS			5**	40

**Note: One Cycle

EROSION CONTROL - MOBILIZATION

PROJECT	628.1905 MOBILIZATIONS EROSION CONTROL EACH	628.1910 MOBILIZATIONS EMERGENCY EROSION CONTROL EACH
5685-00-05	2	2

PAVEMENT MARKING - LINES

STATION	- STATION	LOCATION	646.1020 MARKING LINE EPOXY 4-INCH			646.6120	646.7420	646.7520
			YELLOW SOLID LF	WHITE SOLID LF	WHITE DASHED LF	MARKING STOP LINE EPOXY 18-INCH LF	MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH LF	MARKING CROSSWALK EPOXY BLOCK STYLE 24-INCH LF
12+66	- 56+84	EXCHANGE	8840	10940	215	--	--	--
18+13.56	- 18+13.56	EXCHANGE	--	--	--	--	--	42
21+25.80	- 21+25.80	EXCHANGE	--	--	--	--	--	42
21+96.63	- 21+96.63	EXCHANGE	--	--	--	--	--	42
24+24.29	- 24+24.29	EXCHANGE	--	--	--	--	--	42
24+88.59	- 24+88.59	EXCHANGE	--	--	--	--	--	42
37+61.82	- 37+61.82	EXCHANGE	--	--	--	--	--	42
38+10.80	- 38+18.19	EXCHANGE	--	--	--	--	--	42
44+83.72	- 44+83.72	EXCHANGE	--	--	--	--	--	42
45+39.74	- 45+39.74	EXCHANGE	--	--	--	--	--	42
52+35.84	- 52+35.84	EXCHANGE	--	--	--	--	--	42
53+05.97	- 53+05.97	EXCHANGE	--	--	--	--	--	42
56+84	- 56+84	EXCHANGE	--	--	--	25	--	--
-	-	SLEEPY HOLLOW	--	--	--	--	--	--
-	-	JAEGER	--	--	--	10	--	--
-	-	AHREN	--	60	--	15	60	--
-	-	HIGHLAND	--	80	--	20	80	--
-	-	LEWIS	--	64	--	15	65	--
-	-	RENEE	--	62	--	15	60	--
-	-	SURE	--	62	--	15	60	--
SUBTOTALS			8840	11268	215	115	325	462
TOTALS				20323		115	325	462

PAVEMENT MARKINGS - SYMBOLS

STATION	- STATION	LOCATION	646.5020		646.5220	
			MARKING ARROW EPOXY - TYPE 2 EACH	MARKING ARROW EPOXY - TYPE 3 EACH	MARKING SYMBOL EPOXY - BIKE LANE SYMBOL EACH	MARKING SYMBOL EPOXY - BIKE LANE SYMBOL FOR SHARED LANE EACH
12+66	- 56+84	EXCHANGE	2	2	31	2
SUBTOTALS			2	2	31	2
TOTALS				4		33

TRAFFIC CONTROL PEDESTRIAN ACCOMODATION

FROM	TO	LOCATION	644.1601	644.1605	644.1810	644.1440
			TEMPORARY PEDESTRIAN CURB RAMP EACH	TEMPORARY PEDESTRIAN DETECTABLE WARNING FIELD SF	TEMPORARY PEDESTRIAN BARRICADE LF	TEMPORARY PEDESTRIAN SURFACE MATTING SF
SLEEPY HOLLOW ROAD	- JAGER ROAD	EXCHANGE	4	40	840	2500
JAGER ROAD	- FARWELL STREET	EXCHANGE	20	400	584	5200
TOTALS			24	440	1424	7700

SANITARY SEWER

STRUCTURE	TO	STRUCTURE	SPV.0060.03 EXISTING SANITARY SEWER CONNECTION EACH	SPV.0060.04 SANITARY LATERAL TRACER WIRE BOX EACH	SPV.0060.05 SANITARY SERVICE LATERAL CONNECTION EACH	SPV.0060.06 SANITARY SEWER 18"X4" WYE EACH	SPV.0060.07 SANITARY SEWER 8"X4" WYE EACH
SA321	-	SA323	2	4	4	4	0
SA323	-	SA324	1	5	5	5	0
SA324	-	SA327	0	2	2	2	0
SA327	-	SA328	1	3	3	3	0
SA252	-	SA251	4	1	2	0	2
TOTAL			8	15	16	14	2

SANITARY SEWER - CONTINUED

STRUCTURE	TO	STRUCTURE	SPV.0090.01 PVC 18-INCH LF	SPV.0090.02 PVC 12-INCH LF	SPV.0090.03 PVC 8-INCH LF	SPV.0090.04 PVC 4-INCH LF
SA321	-	SA323	245	6		132
SA323	-	SA324	207			165
SA324	-	SA327	91		5	165
SA327	-	SA328	232	8		99
SA252	-	SA251			20	55
TOTAL			775	14	25	616

SANITARY SEWER - MANHOLES

STATION	STRUCTURE	SPV.0060.08 SANITARY MANHOLE CASTING EACH	SPV.0200.01 SANITARY MANHOLE MASONRY 48-INCH DIAMETER VF
17+65.62	SA321	1	8.10
19+66.63	SA323	1	9.01
21+73.69	SA324	1	7.77
22+65.42	SA327	1	7.70
25+01.80	SA328	1	7.62
26+43.42	SA329	1	0
28+98.35	SA330	1	0
33+01.29	SA331	1	0
36+40.94	SA332	1	0
38+77.12	SA253	1	0
41+32.09	SA252	1	0
45+11.30	SA251	1	0
48+62.38	SA250	1	0
52+54.40	SA247	1	0
54+63.84	SA246	1	0
TOTAL		15	40.2

**TEMPORARY SEWER BYPASSING
AND CONNECTIONS**

STATION	-	STATION	SPV.0090.10 LF
17+65	-	25+02	729

UTILITY LINE OPENING

STATION	-	STATION	SPV.0060.20 EACH
12+73	-	53+73	10

3

3

WATER MAIN

STATION	SPV.0060.09 10" VALVES AND BOXES EACH	SPV.0060.10 8" VALVES AND BOXES EACH	SPV.0060.11 6" VALVES AND BOXES EACH	SPV.0060.12 1" CURB STOPS EACH	SPV.0060.13 1" CORPORATION STOPS EACH
31+10.63 - 56+39.52	17	2	6	49	49
TOTAL	17	2	6	49	49

WATER MAIN - CONTINUED

STATION	SPV.0060.14 WATER SERVICE LATERAL RECONNECTIONS EACH	SPV.0060.15 EXISTING MAIN RECONNECTIONS EACH	SPV.0090.05 12" WATER MAIN, DUCTILE IRON LF	SPV.0090.06 10" WATER MAIN, DUCTILE IRON LF	SPV.0090.07 8" WATER MAIN, DUCTILE IRON LF	SPV.0090.08 6" WATER MAIN, DUCTILE IRON LF	SPV.0090.09 1" COPPER WATER SERVICE LATERALS LF
31+10.63 - 56+39.52	49	13	22	2535	60	200	1372
TOTAL	49	13	22	2535	60	200	1372

HYDRANTS

STATION	SPV.0060.16 NEW HYDRANTS EACH	SPV.0060.17 EXISTING HYDRANT REMOVALS EACH
12+74.63	1	1
18+03.75	-	1
18+20.22	1	-
22+10.75	1	-
22+18.26	-	1
24+01.67	-	1
24+15.65	1	-
34+96.00	1	1
39+53.54	1	1
45+53.60	1	1
50+48.33	1	1
56+35.87	1	1
TOTAL	9	9

SANITARY SEWER SPOT REPAIR

STATION	STATION	LOCATION	SPV.0060.21 EACH
43+82	43+92	EXCHANGE	1
44+23	44+35	EXCHANGE	1
TOTAL			2

CONVENTIONAL SYMBOLS

SECTION LINE	---	SECTION CORNER SYMBOL		RAW MONUMENT (TO BE SET)	●
QUARTER LINE	---	SECTION CORNER MONUMENT		NON-MONUMENTED R/W POINT	○
SIXTEENTH LINE	---	SECTION CORNER MONUMENT		FOUND IRON PIN (1-INCH UNLESS NOTED)	IP
NEW REFERENCE LINE	---	SECTION CORNER MONUMENT		GEODETIC SURVEY MONUMENT	
NEW R/W LINE	---	SECTION CORNER MONUMENT		SIXTEENTH CORNER MONUMENT	
EXISTING R/W OR HE LINE	---	SIGN		OFF-PREMISE SIGN	
PROPERTY LINE	---	PARALLEL OFFSETS			
LOT, TIE & OTHER MINOR LINES	---	PARALLEL OFFSETS			
SLOPE INTERCEPT	---	PARALLEL OFFSETS			
CORPORATE LIMITS	---	PARALLEL OFFSETS			
UNDERGROUND FACILITY (COMMUNICATIONS, ELECTRIC, ETC)	---	PARALLEL OFFSETS			
NEW R/W (FEE OR HE) (HATCHING VARIES BY OWNER)	---	PARALLEL OFFSETS			
TEMPORARY LIMITED EASEMENT AREA	---	PARALLEL OFFSETS			
EASEMENT AREA (PERMANENT LIMITED OR RESTRICTED DEVELOPMENT)	---	PARALLEL OFFSETS			
TRANSMISSION STRUCTURES	---	PARALLEL OFFSETS			
BUILDING		PARALLEL OFFSETS			
TO BE REMOVED		PARALLEL OFFSETS			
BRIDGE		PARALLEL OFFSETS			
CULVERT		PARALLEL OFFSETS			

CONVENTIONAL ABBREVIATIONS

ACCESS RIGHTS	AR	PAGE	P
ACRES	AC	POINT OF TANGENCY	PT
AHEAD	AH	PERMANENT LIMITED EASEMENT	PLE
ALUMINUM	ALUM	POINT OF BEGINNING	POB
AND OTHERS	ET AL	POINT OF CURVATURE	PC
BACK	BK	POINT OF COMPOUND CURVE	PCC
BLOCK	BLK	POINT OF INTERSECTION	PI
CENTERLINE	C/L	PROPERTY LINE	PL
CERTIFIED SURVEY MAP	CSM	RECORDED AS (100')	PL
CONCRETE	CONC	REEL / IMAGE	R/I
COUNTY	CO	REFERENCE LINE	R/L
COUNTY TRUNK HIGHWAY	CTH	REMAINING	REM
DISTANCE	DIST	RESTRICTIVE DEVELOPMENT EASEMENT	RDE
CORNER	COR	RIGHT	RT
DOCUMENT NUMBER	DOC	RIGHT OF WAY	R/W
EASEMENT	EASE	SECTION	SEC
EXISTING	EX	SEPTIC VENT	SEPV
GAS VALVE	GV	SQUARE FEET	SF
GRID NORTH	GN	STATE TRUNK HIGHWAY	STH
HIGHWAY EASEMENT	HE	STATION	STA
IDENTIFICATION	ID	TELEPHONE PEDESTAL	TP
LAND CONTRACT	LC	TEMPORARY LIMITED EASEMENT	TLE
LEFT	LT	TRANSPORTATION PROJECT PLAT	TPP
MONUMENT	MON	UNITED STATES HIGHWAY	USH
NATIONAL GEODETIC SURVEY NUMBER	NGS	VOLUME	V
OUTLOT	OL		

CONVENTIONAL UTILITY SYMBOLS

WATER	W
GAS	G
TELEPHONE	T
OVERHEAD TRANSMISSION LINES	OH
ELECTRIC	E
CABLE TELEVISION	TV
FIBER OPTIC	FO
SANITARY SEWER	SS
STORM SEWER	SS
ELECTRIC TOWER	ET

CURVE DATA ABBREVIATIONS

LONG CHORD	LCH
LONG CHORD BEARING	LCB
RADIUS	R
DEGREE OF CURVE	D
CENTRAL ANGLE	Δ / DELTA
LENGTH OF CURVE	L
TANGENT	T
DIRECTION AHEAD	DA
DIRECTION BACK	DB

NOTES

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN COUNTY COORDINATES, DANE COUNTY, NAD 83 (2011) IN US SURVEY FEET. VALUES SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 MONUMENTS (TYPICALLY 3/4" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS OF PUBLIC RECORD."

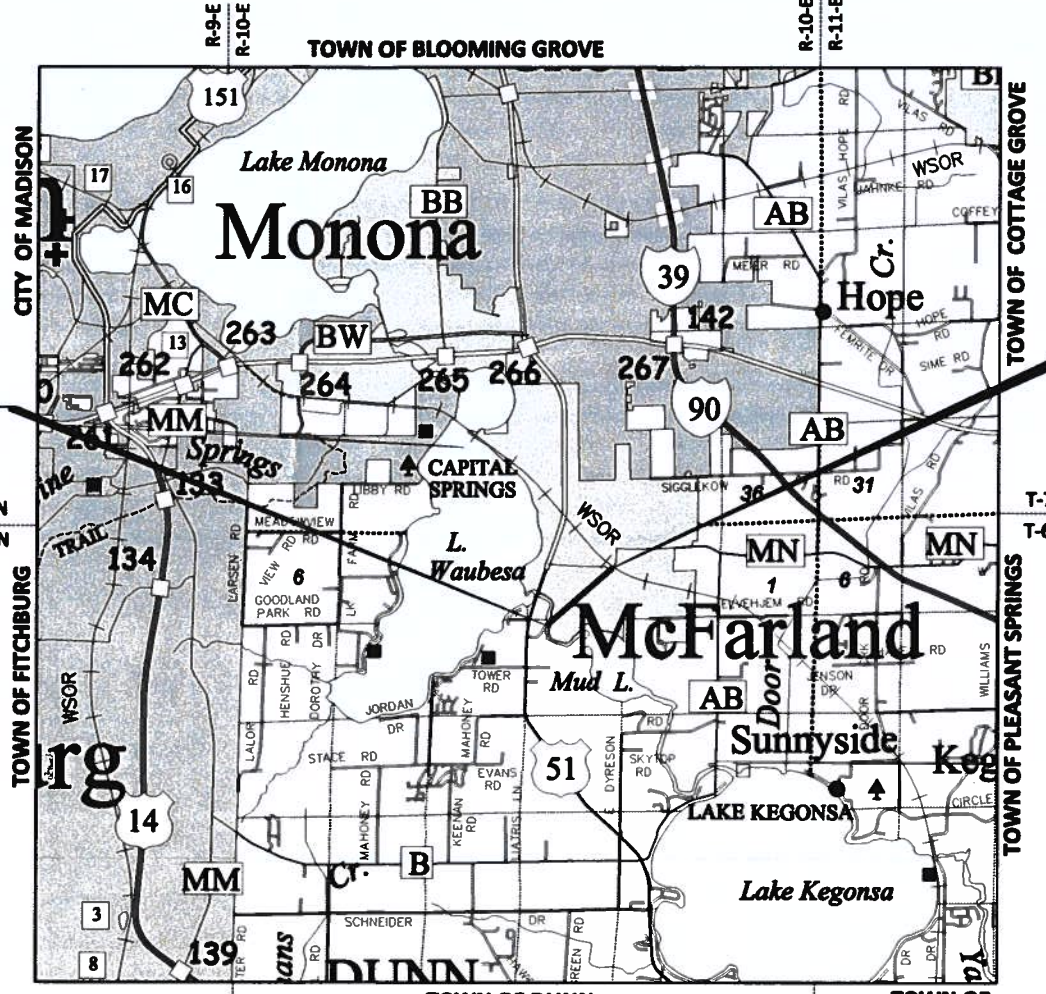
ALL RIGHT-OF-WAY LINES DEPICTED IN THE NON-ACQUISITION AREAS ARE INTENDED TO RE-ESTABLISH EXISTING RIGHT-OF-WAY LINES AS DETERMINED FROM PREVIOUS PROJECTS, OTHER RECORDED DOCUMENTS, OR FROM CENTERLINE OF EXISTING PAVEMENTS.

DIMENSIONING FOR THE NEW RIGHT-OF-WAY IS MEASURED ALONG AND PERPENDICULAR TO THE NEW REFERENCE LINES.

A TEMPORARY LIMITED EASEMENT (TLE) IS A RIGHT FOR CONSTRUCTION PURPOSES, AS DEFINED HEREIN, INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON, THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM DESIRABLE. ALL (TLE's) ON THIS PLAT EXPIRE AT THE COMPLETION OF THE CONSTRUCTION PROJECT FOR WHICH THIS INSTRUMENT IS GIVEN.

THIS PLAT IS A GRAPHIC REPRESENTATION AND IS FOR REFERENCE PURPOSE ONLY. DEEDS MUST BE CHECKED TO DETERMINE PROPERTY BOUNDARIES AND ACCESS RIGHTS.

FOR CURRENT ACCESS/DRIVEWAY INFORMATION, CONTACT THE VILLAGE OF McFARLAND.



BEGIN RELOCATION ORDER
 STA. 12+51.00
 1298.11' EAST AND 175.22' SOUTH OF THE NORTHWEST CORNER OF SECTION 10, T.6N., R.10E., VILLAGE OF McFARLAND, DANE COUNTY, WI
 Y = 457287.948
 X = 843144.221

END RELOCATION ORDER
 STA. 56+50.00
 872.60' WEST AND 23.57' NORTH OF THE EAST QUARTER CORNER OF SECTION 3, T.6N., R.10E., VILLAGE OF McFARLAND, DANE COUNTY, WI
 Y = 460139.292
 X = 846363.661

LAYOUT
 SCALE 0 2 MI.
 TOTAL NET LENGTH OF CENTERLINE = 0.833 MI.

STATE PROJECT NUMBER	5685-00-04/05	SHEET NUMBER	4.01	TOTAL SHEETS	6
FEDERAL PROJECT NUMBER					
PLAT OF RIGHT-OF-WAY REQUIRED FOR VILLAGE OF McFARLAND (SLEEPY HOLLOW ROAD TO FARWELL STREET)					
EXCHANGE STREET			DANE COUNTY		
CONSTRUCTION PROJECT NUMBER					

JEWELL
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 Engineers - Architects - Surveyors
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 PHONE : 608.588.7484
 www.jewellassoc.com

I HEREBY CERTIFY THAT THIS PLAT WAS MADE FOR VILLAGE OF McFARLAND, WISCONSIN AND IS CORRECT TO THE BEST OF MY KNOWLEDGE AND BELIEF.

WISCONSIN
 GREG A. JEWELL
 S-1898
 DODGEVILLE, WI
 LAND SURVEYOR

DATE: 10/13/2022
 Matthew G. Schwenke, Village Administrator

REVISION DATE
 7-27-2023 N.C.
 10-10-2023 N.C.

APPROVED FOR VILLAGE OF McFARLAND

SCHEDULE OF LANDS & INTERESTS REQUIRED							
PARCEL NUMBER	SHEET NUMBER	OWNER (S)	INTEREST REQUIRED	R/W ACRES REQUIRED			TLE ACRES REQUIRED
				NEW	EXISTING	TOTAL	
4	4.03	KATHRYN A. BROWN	TLE	0	0	0	0.010
5	4.03	STACIE A. BURSE	TLE	0	0	0	0.010
6	4.03	STEFAN J. BOYLES AND KRISTAN K. MALCOOK	TLE	0	0	0	0.020
7	4.03	JULIE A. PATCH	TLE	0	0	0	0.020
8	4.03	BRIAN M. BURKE AND MELISSA K. BURKE	TLE	0	0	0	0.020
9	4.03	RICHARD W. SABATKE	TLE	0	0	0	0.010
10	4.03	JASON THEODORE BRUNKOW AND TAYLOR OLIVIA BRUNKOW	TLE	0	0	0	0.010
11	4.03	VILLAGE OF MCFARLAND	TLE	0	0	0	0.010
12	4.03 & 4.04	MCFARLAND AND JT SCHOOL DISTRICT 8	TLE	0	0	0	0.050
13	4.03	CHRISTOPHER SPANOS AND ARLENE M. SPANOS	TLE	0	0	0	0.010
14	4.03	DAVID W. KLOTH	TLE	0	0	0	0.020
15	4.03	JENNEKE AND MARTINELLI INCOME TRUST	TLE	0	0	0	0.020
16	4.03	GOESER LIVING TRUST	TLE	0	0	0	0.020
17	4.03	JAMES C. MCKAY III AND JANET E. MCKAY	TLE	0	0	0	0.010
18	4.03	JOSEPH AND DAWN DeFOER	TLE	0	0	0	0.020
19	4.03	MEINDA J. BURNS	TLE	0	0	0	0.010
20	4.03	DONALD SIEBERT AND CYNTHIA SIEBERT	TLE	0	0	0	0.010
21	4.03 & 4.04	LINDSAY J. LOHAUS	TLE	0	0	0	0.023
22	4.04	NANCY DERANICK	TLE	0	0	0	0.020
23	4.04	PAUL R. WEBBER	TLE	0	0	0	0.010
24	4.04	JOSEPH A. TORBLEAU	TLE	0	0	0	0.010
25	4.04	PATRICK J. SKEFFINGTON AND JANET M. SKEFFINGTON	TLE	0	0	0	0.010
26	4.04	RICHARD N. DOMENGET AND JUDITH A. DOMENGET	TLE	0	0	0	0.010
27	4.04	MARY ANN RIEGRT	TLE	0	0	0	0.010
28	4.04	RALPH W. JENSEN	TLE	0	0	0	0.010
29	4.04	KENNETH E. BROST AND EILEEN BROST	TLE	0	0	0	0.030
30	4.04	JEFFERY E. SCHRAML AND MONICA SCHRAML	TLE	0	0	0	0.030
31	4.04	DEBORAH A. NYGAARD	TLE	0	0	0	0.010
32	4.04	DOUGLAS B. WAGNER	TLE	0	0	0	0.010
33	4.04	CRAIG D. DARTSCH AND LISA L. DARTSCH	TLE	0	0	0	0.020
34	4.04	BRADLEY C. HARRISON AND JULIE L. HARRISON	TLE	0	0	0	0.020
35	4.04	MICHAEL S. DREWES AND LORI A. DREWES	TLE	0	0	0	0.020
36	4.04	RYAN J. PETERSEN AND KATHERINE E. RYDER	TLE	0	0	0	0.020
37	4.04	SCOTT A. EDWARDS	TLE	0	0	0	0.010
38	4.04 & 4.05	CHARLES B. DEADMAN AND JACQUEYN L. DEADMAN	TLE	0	0	0	0.012
39	4.04 & 4.05	ERIC J. DAHL	TLE	0	0	0	0.012
40	4.05	ADAM KNUTSON AND ELIZABETH J. HANSON	TLE	0	0	0	0.010
41	4.05	JANICE E. HANSON	TLE	0	0	0	0.020
42	4.05	MCFARLAND HIGH SCHOOL	TLE	0	0	0	0.033
44	4.05	JACOB TISUE AND KORRENA TISUE	TLE	0	0	0	0.010
45	4.05	FORWARD INVESTMENT PROPERTIES LLC	TLE	0	0	0	0.010
46	4.05	DOMINI LIVING TRUST	TLE	0	0	0	0.010
47	4.05	JEFFERY G. LONG	TLE	0	0	0	0.010
48	4.05	ROBERT L. BOLLE REVOCABLE TRUST ROBERT L. AND MONICA R. BOLLE	TLE	0	0	0	0.010
49	4.05	ROMEL SANTINI AND NATALIE R. SANTINI	TLE	0	0	0	0.010
50	4.05	EGNER LIVING TRUST, JEANETTE I	TLE	0	0	0	0.010
51	4.05	URSO PROPERTIES, INC	TLE	0	0	0	0.010
52	4.05 & 4.06	ANN M. FORD	TLE	0	0	0	0.027
53	4.05	JAMES D. GARDNER AND MOLLY M. PECHMANN	TLE	0	0	0	0.020
54	4.05	MICHAEL A. POQUETTE AND CAROL POQUETTE	TLE	0	0	0	0.010
55	4.05	LORI A. SPATOLA AND LESLEE K. RINEHART	TLE	0	0	0	0.020
56	4.05	STEPHANIE F. ANDERSON	TLE	0	0	0	0.010
57	4.05	AUSTIN LIPINSKI AND JESSICA KLABAK	TLE	0	0	0	0.010
58	4.05	KAREN ENGLE	TLE	0	0	0	0.010
59	4.05	ERICH W. RICHARDSON	TLE	0	0	0	0.010
60	4.05	JOHN W. CHERAMY AND TERESA E. CHEREMY	TLE	0	0	0	0.010
61	4.05	ALAN R. ZELLER AND NANCY C. ZELLER	TLE	0	0	0	0.010
62	4.05	BRENDA M. BERLOWSKI-ZIER AND GREGORY B. ZIER	TLE	0	0	0	0.010
63	4.05	LARSON FAMILY TRUST, LYLE L. LARSON REVOCABLE TRUST, ELSIE M.	TLE	0	0	0	0.010
64	4.06	TIMOTHY FIOCCHI	TLE	0	0	0	0.010
65	4.06	DALE W. BECK	TLE	0	0	0	0.020
66	4.05	HEATHER L. CLANCY & JACQUELINE L. PAULY	TLE	0	0	0	0.010
67	4.05 & 4.06	DREW MATHER & ALECIA MATHER	TLE	0	0	0	0.008
200	4.03	ALLIANT ENERGY	TEMPORARY CONTRUCTION EASEMENT				
201	4.03	VILLAGE OF MCFARLAND	TEMPORARY CONTRUCTION EASEMENT				

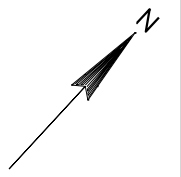
NOTE: OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO VILLAGE OF MCFARLAND.

REVISION DATE 7-27-2023 10-10-2023	DATE October 10, 2023	SCALE, FEET 0 50 100	HWY: EXCHANGE STREET	STATE R/W PROJECT NUMBER 5685-00-04/05	PLAT SHEET 4.02
			COUNTY: DANE	CONSTRUCTION PROJECT NUMBER	PS&E SHEET

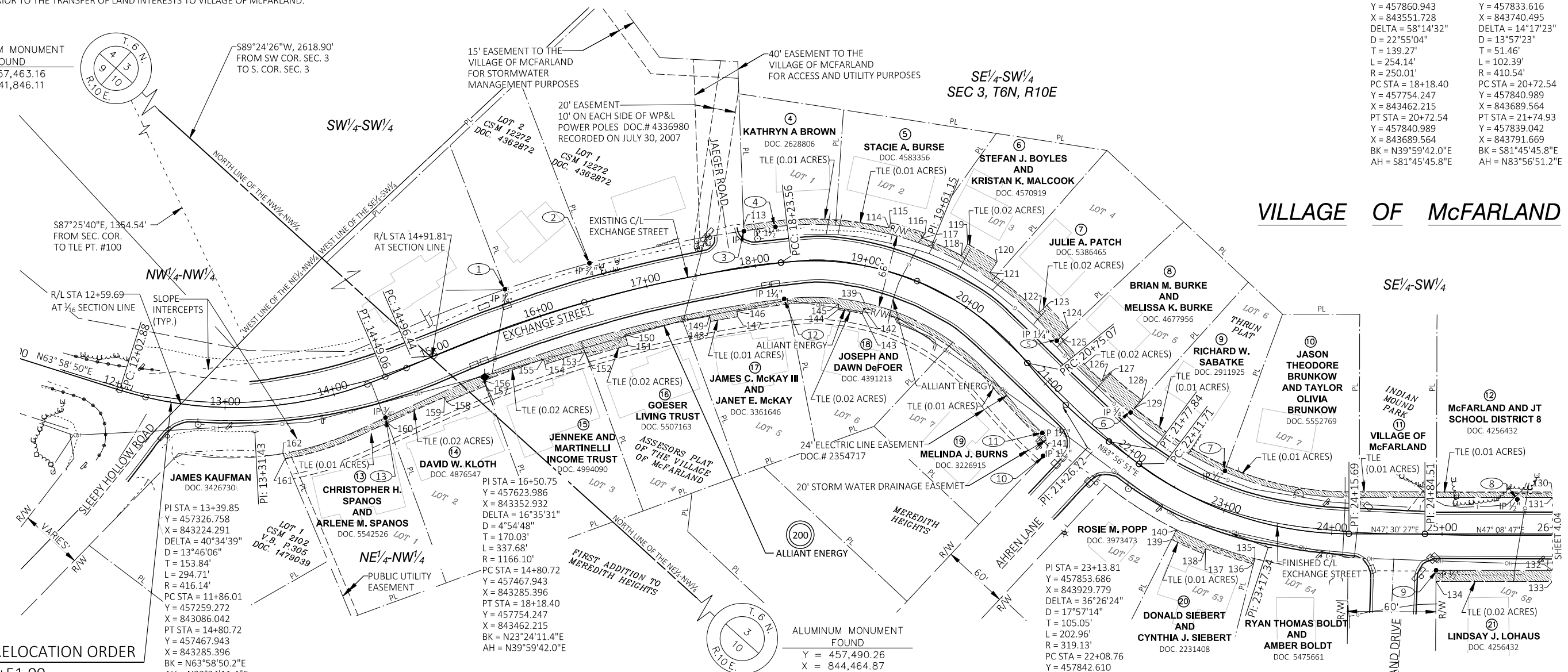
NOTE: OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO VILLAGE OF MCFARLAND.

PI STA = 19+57.67
 Y = 457860.943
 X = 843551.728
 DELTA = 58°14'32"
 D = 22°55'04"
 T = 139.27'
 L = 254.14'
 R = 250.01'
 PC STA = 18+18.40
 Y = 457754.247
 X = 843462.215
 PT STA = 20+72.54
 Y = 457840.989
 X = 843689.564
 BK = N39°59'42.0"E
 AH = S81°45'45.8"E

PI STA = 21+24.00
 Y = 457833.616
 X = 843740.495
 DELTA = 14°17'23"
 D = 13°57'23"
 T = 51.46'
 L = 102.39'
 R = 410.54'
 PC STA = 20+72.54
 Y = 457840.989
 X = 843689.564
 PT STA = 21+74.93
 Y = 457839.042
 X = 843791.669
 BK = S81°45'45.8"E
 AH = N83°56'51.2"E



ALUMINUM MONUMENT FOUND
 Y = 457,463.16
 X = 841,846.11



BEGIN RELOCATION ORDER

STA. 12+51.00
 1298.11' EAST AND 175.22' SOUTH OF THE NORTHWEST CORNER OF SECTION 10, T.6N., R.10E., VILLAGE OF MCFARLAND, DANE COUNTY, WI
 Y = 457287.948
 X = 843144.221

PT#	STATION	OFFSET	Y	X
113	17+94.90	38.03 L	457757.987	843415.514
114	19+13.00	38.00 L	457845.692	843513.909
115	19+13.00	43.00 L	457850.068	843511.490
116	19+41.08	43.00 L	457864.353	843541.129
117	19+39.91	38.00 L	457859.213	843541.794
118	19+74.00	38.00 L	457871.907	843578.937
119	19+74.00	50.00 L	457883.500	843575.840
120	20+00.00	50.00 L	457889.971	843606.354
121	20+00.00	38.00 L	457878.119	843608.229
122	20+50.00	38.00 L	457881.383	843665.648

PT#	STATION	OFFSET	Y	X
123	20+50.00	48.00 L	457891.374	843666.085
124	20+74.95	48.00 L	457888.604	843695.683
125	20+75.74	38.00 L	457878.605	843694.964
126	21+36.00	38.00 L	457874.855	843749.527
127	21+36.00	48.00 L	457884.855	843749.488
128	21+79.20	48.00 L	457887.008	843787.739
129	21+78.27	38.00 L	457876.964	843787.865
130	26+00.00	38.00 L	458078.557	844117.253
131	26+00.00	33.00 L	458074.889	844120.645
132	22+97.00	33.03 R	457832.118	843919.780
133	22+97.00	43.00 R	457822.833	843923.418
134	22+68.98	43.00 R	457812.548	843893.335
135	22+69.00	33.02 R	457822.127	843890.544
136	21+38.58	38.00 R	457798.876	843752.651
137	20+07.65	38.00 R	457803.968	843626.525
138	20+08.41	33.00 R	457809.024	843626.551
139	19+07.00	33.03 R	457780.972	843543.776
140	19+07.00	43.00 R	457772.360	843548.806
141	18+80.00	43.00 R	457760.055	843530.165
142	18+80.00	33.03 R	457768.068	843524.230

PT#	STATION	OFFSET	Y	X
143	17+77.00	33.01 R	457699.774	843460.010
144	17+77.00	43.00 R	457693.627	843467.880
145	17+51.00	43.00 R	457673.732	843452.704
146	17+51.00	38.00 R	457676.718	843448.694
147	16+70.00	38.00 R	457612.317	843404.162
148	16+70.00	48.00 R	457606.923	843412.583
149	16+29.65	48.00 R	457574.004	843392.306
150	16+29.33	43.00 R	457576.286	843387.846
151	15+90.00	43.00 R	457543.380	843369.135
152	15+90.00	38.00 R	457545.776	843364.747
153	15+30.71	38.00 R	457494.779	843338.583
154	15+30.58	43.00 R	457492.501	843343.035
155	14+91.00	43.00 R	457457.712	843327.036
156	14+91.00	38.00 R	457459.743	843322.467
157	14+35.90	38.00 R	457408.191	843299.248
158	14+35.83	43.00 R	457405.913	843303.700
159	14+09.00	43.00 R	457379.382	843289.371
160	14+09.00	37.36 R	457382.250	843284.519
161	13+48.83	48.89 R	457320.601	843254.549
162	13+47.56	43.96 R	457322.721	843249.878

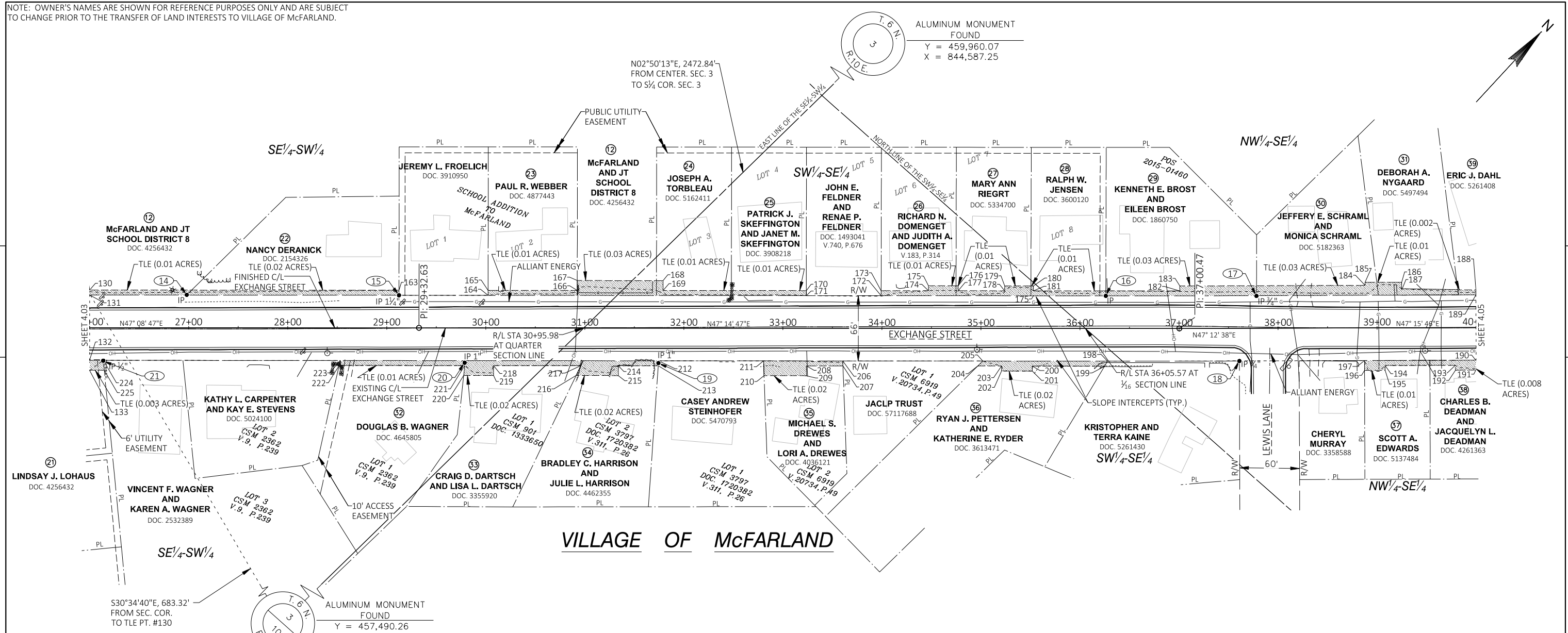
FOUND MONUMENTS				
PT#	STATION	OFFSET	Y	X
1	15+77.64	33.08 L	457568.660	843296.330
2	16+55.53	32.98 L	457638.020	843336.440
3	17+94.32	32.64 L	457754.133	843419.336
4	18+22.39	32.91 L	457776.573	843437.516
5	20+76.08	33.51 L	457874.113	843694.646
6	21+77.76	32.85 L	457871.793	843787.936
7	22+86.49	33.79 L	457891.053	843886.606
8	25+68.15	31.31 L	458052.023	844098.406
9	24+94.66	32.99 R	457954.920	844088.110
10	21+41.96	48.07 R	457788.860	843756.520
11	21+28.59	33.07 R	457803.830	843741.810
12	18+22.40	32.97 R	457734.013	843487.796
13	14+35.89	34.36 R	457409.781	843295.973

NOTE: EXISTING C/L OF EXCHANGE STREET BASED ON THE LOCATION OF THE EXISTING PAVEMENT.

EXISTING RIGHT-OF-WAY FOR EXCHANGE STREET BASED ON THE LOCATION OF THE C/L OF THE EXISTING PAVEMENT, COUNTY RECORDS, ASSOCIATED FOUND SURVEY MONUMENTATION, AND WS. STATUTE 82.31(2).

REVISION DATE 7-27-2023 10-10-2023 N.C.	DATE October 10, 2023	SCALE, FEET 0 50 100	HWY: EXCHANGE STREET COUNTY: DANE	STATE R/W PROJECT NUMBER 5685-00-04/05 CONSTRUCTION PROJECT NUMBER	PLAT SHEET 4.03 PS&E SHEET
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FOUND MONUMENTS				
PT#	STATION	OFFSET	Y	X
14	26+98.17	33.24 L	458141.713	844192.556
15	29+12.43	33.00 L	458287.003	844350.036
16	36+26.09	32.95 L	458771.470	844874.060
17	37+78.39	32.70 L	458874.830	844985.720
18	37+60.86	33.05 R	458814.798	845017.747
19	31+74.41	35.37 R	458414.668	844588.807
20	29+78.75	35.15 R	458281.991	844444.992
21	26+13.76	33.38 R	458035.490	844175.820

TLE POINTS STATION OFFSET TABLE				
PT#	STATION	OFFSET	Y	X
130	26+00.00	38.00 L	458078.557	844117.253
131	26+00.00	33.00 R	458074.889	844120.645
132	26+00.00	33.00 R	458026.431	844165.455
133	26+00.00	43.00 R	458019.086	844172.246
163	29+12.42	38.00 L	458290.670	844346.637
164	30+02.42	32.99 L	458348.095	844416.116
165	30+02.42	38.00 L	458351.771	844412.717
166	30+92.42	38.00 L	458412.871	844478.799
167	30+92.42	48.00 L	458420.214	844472.010
168	31+79.00	48.00 L	458478.991	844535.578
169	31+79.00	38.00 L	458471.649	844542.367
170	33+23.65	38.00 L	458569.848	844648.571
171	33+23.64	33.00 L	458566.172	844651.965
172	33+99.25	33.00 L	458617.505	844707.480
173	33+99.26	38.00 L	458621.179	844704.089
174	34+47.00	38.00 L	458653.589	844739.140
175	34+47.00	43.00 L	458657.262	844735.744
176	34+77.00	43.00 L	458677.629	844757.771
177	34+76.99	38.00 L	458673.955	844761.166
178	35+24.00	38.00 L	458705.870	844795.682
179	35+24.00	43.00 L	458709.542	844792.286
180	35+53.00	43.00 L	458729.225	844813.573

TLE POINTS STATION OFFSET TABLE				
PT#	STATION	OFFSET	Y	X
182	37+01.00	38.00 L	458825.920	844925.512
183	37+01.00	43.00 L	458829.577	844922.102
184	38+88.00	43.00 L	458957.126	845058.851
185	38+88.00	46.00 L	458959.319	845056.805
186	39+25.00	46.00 L	458984.556	845083.861
187	39+25.00	38.00 L	458978.708	845089.321
188	40+00.00	37.95 L	459029.828	845144.200
189	40+00.00	33.00 L	459026.208	845147.577
190	40+00.00	33.00 R	458977.943	845192.594
191	40+00.00	43.00 R	458970.631	845199.415
192	39+79.00	43.00 R	458956.304	845184.055
193	39+79.00	38.00 R	458959.960	845180.644
194	39+08.00	38.00 R	458911.536	845128.727
195	39+08.00	43.00 R	458907.879	845132.137
196	38+87.26	43.00 R	458893.735	845116.973
197	38+87.26	33.00 R	458901.048	845110.153
198	36+27.96	33.00 R	458724.318	844920.214
199	36+24.99	38.00 R	458718.632	844921.427
200	35+52.00	38.00 R	458669.073	844867.832
201	35+52.00	43.00 R	458665.403	844871.225
202	35+21.00	43.00 R	458644.363	844848.468
203	35+21.00	38.00 R	458648.031	844845.075

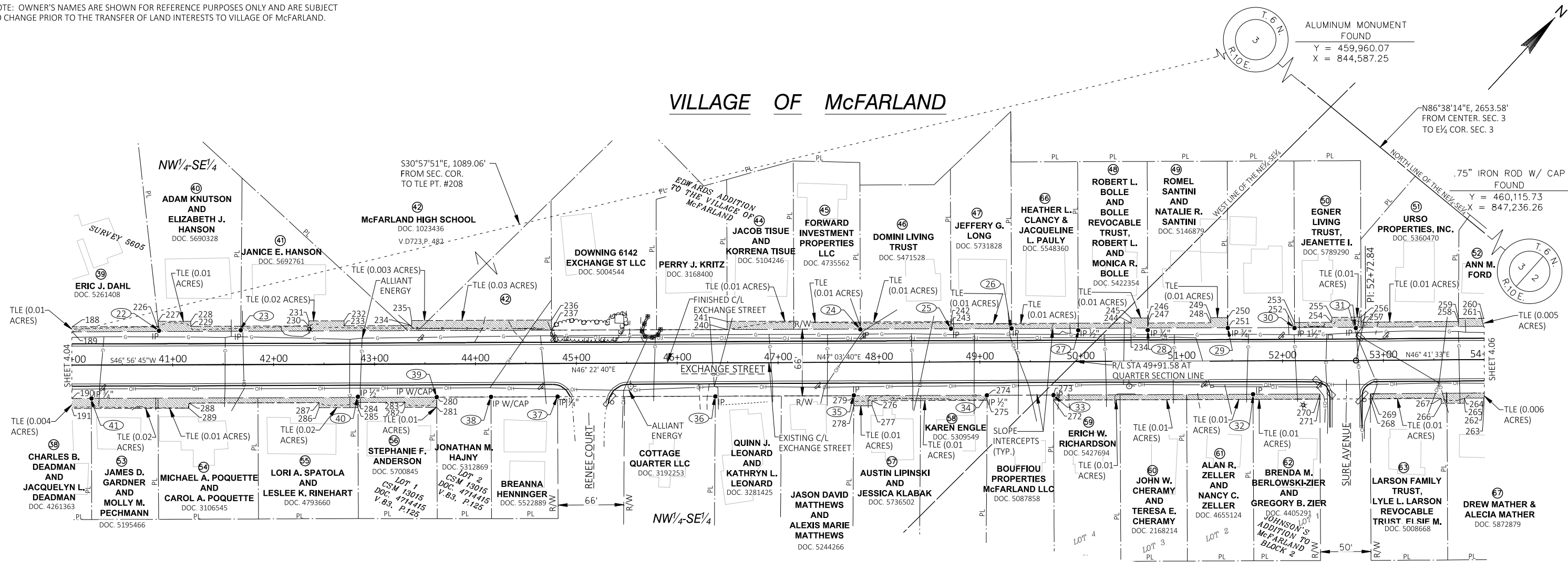
TLE POINTS STATION OFFSET TABLE				
PT#	STATION	OFFSET	Y	X
204	34+96.90	38.00 R	458631.667	844827.377
205	35+01.95	33.00 R	458638.768	844827.692
206	33+60.63	33.00 R	458542.821	844723.926
207	33+60.81	38.00 R	458539.276	844727.452
208	33+24.01	38.00 R	458514.289	844700.433
209	33+24.00	48.00 R	458506.945	844707.216
210	32+81.11	48.00 R	458477.825	844675.723
211	32+80.56	33.00 R	458488.462	844665.137
212	31+74.57	33.00 R	458416.506	844587.317
213	31+74.22	38.00 R	458412.601	844590.459
214	31+34.00	38.01 R	458385.295	844560.928
215	31+34.00	48.00 R	458377.956	844567.713
216	30+91.33	48.00 R	458348.985	844536.380
217	30+96.14	38.01 R	458359.589	844533.127
218	30+08.00	38.00 R	458299.757	844468.410
219	30+08.00	48.00 R	458292.415	844475.199
220	29+77.76	48.00 R	458271.887	844452.998
221	29+78.83	38.00 R	458279.952	844446.991
222	28+52.03	38.00 R	458193.867	844353.894
223	28+53.15	33.00 R	458198.298	844351.327
224	26+13.88	33.00 R	458035.852	844175.643
225	26+14.86	43.00 R	458029.172	844183.153

NOTE: EXISTING C/L OF EXCHANGE STREET BASED ON THE LOCATION OF THE EXISTING PAVEMENT.
EXISTING RIGHT-OF-WAY FOR EXCHANGE STREET BASED ON THE LOCATION OF THE C/L OF THE EXISTING PAVEMENT, COUNTY RECORDS, ASSOCIATED FOUND SURVEY MONUMENTATION, AND WS. STATUTE 82.31(2).

REVISION DATE 7-27-2023 N.C. 10-10-2023 N.C.	DATE October 10, 2023	SCALE, FEET 0 50 100	HWY: EXCHANGE STREET COUNTY: DANE	STATE R/W PROJECT NUMBER 5685-00-04/05 CONSTRUCTION PROJECT NUMBER	PLAT SHEET 4.04 PS&E SHEET	E
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NOTE: OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO VILLAGE OF MCFARLAND.

VILLAGE OF MCFARLAND



PT#	STATION	OFFSET	Y	X
188	40+00.00	37.95	L 459029.828	845144.200
189	40+00.00	33.00	L 459026.208	845147.577
190	40+00.00	33.00	R 458977.943	845192.594
191	40+00.00	43.00	R 458970.631	845199.415
226	40+86.66	33.01	L 459085.324	845210.944
227	40+85.59	43.00	L 459091.897	845203.343
228	41+18.00	43.00	L 459114.004	845227.045
229	41+18.00	38.00	L 459110.347	845230.455
230	42+35.00	38.00	L 459190.156	845316.020
231	42+35.00	43.00	L 459193.813	845312.610
232	42+70.00	43.02	L 459217.701	845338.188
233	42+70.00	38.00	L 459214.028	845341.614
234	43+37.00	38.00	L 459259.723	845390.605
235	43+37.00	43.00	L 459263.379	845387.194
236	44+75.66	43.00	L 459357.957	845488.594
237	44+75.66	33.00	L 459350.644	845493.414
240	46+49.76	33.00	L 459469.395	845622.730
241	46+49.76	41.00	L 459475.245	845617.274
242	48+71.53	40.00	L 459625.781	845780.136
243	48+71.53	38.00	L 459624.320	845781.499
244	50+50.00	38.00	L 459746.051	845912.011
245	50+50.00	42.98	L 459749.691	845908.616
246	50+66.60	42.98	L 459761.014	845920.756
247	50+66.60	38.00	L 459757.374	845924.150
248	51+29.00	38.00	L 459799.931	845969.778
249	51+29.00	43.00	L 459803.588	845966.367

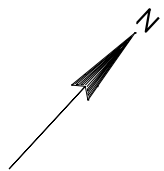
PT#	STATION	OFFSET	Y	X
249	51+29.00	43.00	L 459803.588	845966.367
250	51+45.96	43.02	L 459815.173	845978.755
251	51+45.96	33.00	L 459807.823	845985.569
252	52+12.06	33.00	L 459852.928	846033.927
253	52+12.06	38.00	L 459856.584	846030.517
254	52+49.00	38.00	L 459881.783	846057.533
255	52+49.00	43.00	L 459885.439	846054.122
256	52+78.24	43.00	L 459905.275	846075.355
257	52+78.22	38.00	L 459901.618	846078.766
258	53+74.64	38.00	L 459967.691	846148.993
259	53+74.69	41.00	L 459969.904	846146.968
260	54+00.00	41.00	L 459987.250	846165.404
261	54+00.00	33.00	L 459981.423	846170.886
262	54+00.00	33.00	L 459933.353	846216.111
263	54+00.00	38.00	R 459929.712	846219.538
264	53+80.00	38.00	R 459916.010	846204.974
265	53+80.00	43.00	R 459912.368	846208.400
266	53+57.00	43.00	R 459896.605	846191.645
267	53+57.00	38.00	R 459900.249	846188.222
268	52+86.75	38.00	R 459852.110	846137.055
269	52+86.77	33.00	R 459855.763	846133.641
270	52+36.91	33.00	R 459821.615	846097.118
271	52+36.91	38.00	R 459817.962	846100.533
272	49+72.91	38.00	R 459637.893	845907.475
273	49+72.90	33.00	R 459641.542	845904.058
274	49+07.26	33.00	R 459596.768	845856.053

PT#	STATION	OFFSET	Y	X
275	49+07.21	38.00	R 459593.076	845859.426
276	47+93.00	38.00	R 459515.178	845775.909
277	47+93.00	43.00	R 459511.521	845779.319
278	47+74.92	43.00	R 459499.192	845766.099
279	47+75.03	33.00	R 459506.577	845759.358
280	43+61.56	33.00	R 459224.560	845456.998
281	43+61.58	43.00	R 459217.255	845463.828
282	43+30.00	43.00	R 459195.718	845440.737
283	43+30.00	38.00	R 459199.374	845437.327
284	42+83.50	38.00	R 459167.658	845403.324
285	42+83.50	43.00	R 459164.002	845406.734
286	42+45.00	43.00	R 459137.738	845378.575
287	42+45.00	38.00	R 459141.394	845375.165
288	41+16.00	38.00	R 459053.407	845280.831
289	41+16.00	43.00	R 459049.751	845284.242

PT#	STATION	OFFSET	Y	X
22	40+86.58	33.76	L 459085.820	845210.370
23	41+67.71	34.04	L 459141.360	845269.510
24	47+81.76	32.63	L 459559.160	845719.510
25	48+71.53	33.16	L 459620.774	845784.801
26	49+31.60	33.12	L 459661.720	845828.750
27	49+97.66	31.04	L 459705.260	845878.480
28	50+66.60	30.99	L 459752.250	845928.930
29	51+45.93	32.76	L 459807.650	845985.730
30	52+12.06	32.48	L 459852.550	846034.280
31	52+73.01	32.24	L 459893.850	846078.920
32	51+70.72	32.97	R 459776.490	846048.690
33	49+72.91	33.13	R 459641.450	845904.150
34	49+06.88	33.05	R 459596.470	845855.810
35	47+75.04	32.72	R 459506.786	845759.169
36	46+37.76	33.29	R 459412.736	845659.169
37	44+81.41	32.93	R 459306.354	845544.591
38	44+15.41	32.87	R 459261.384	845496.281
39	43+61.56	32.95	R 459224.594	845456.961
40	42+83.51	32.30	R 459171.829	845399.442
41	40+19.43	33.06	R 458991.155	845206.841

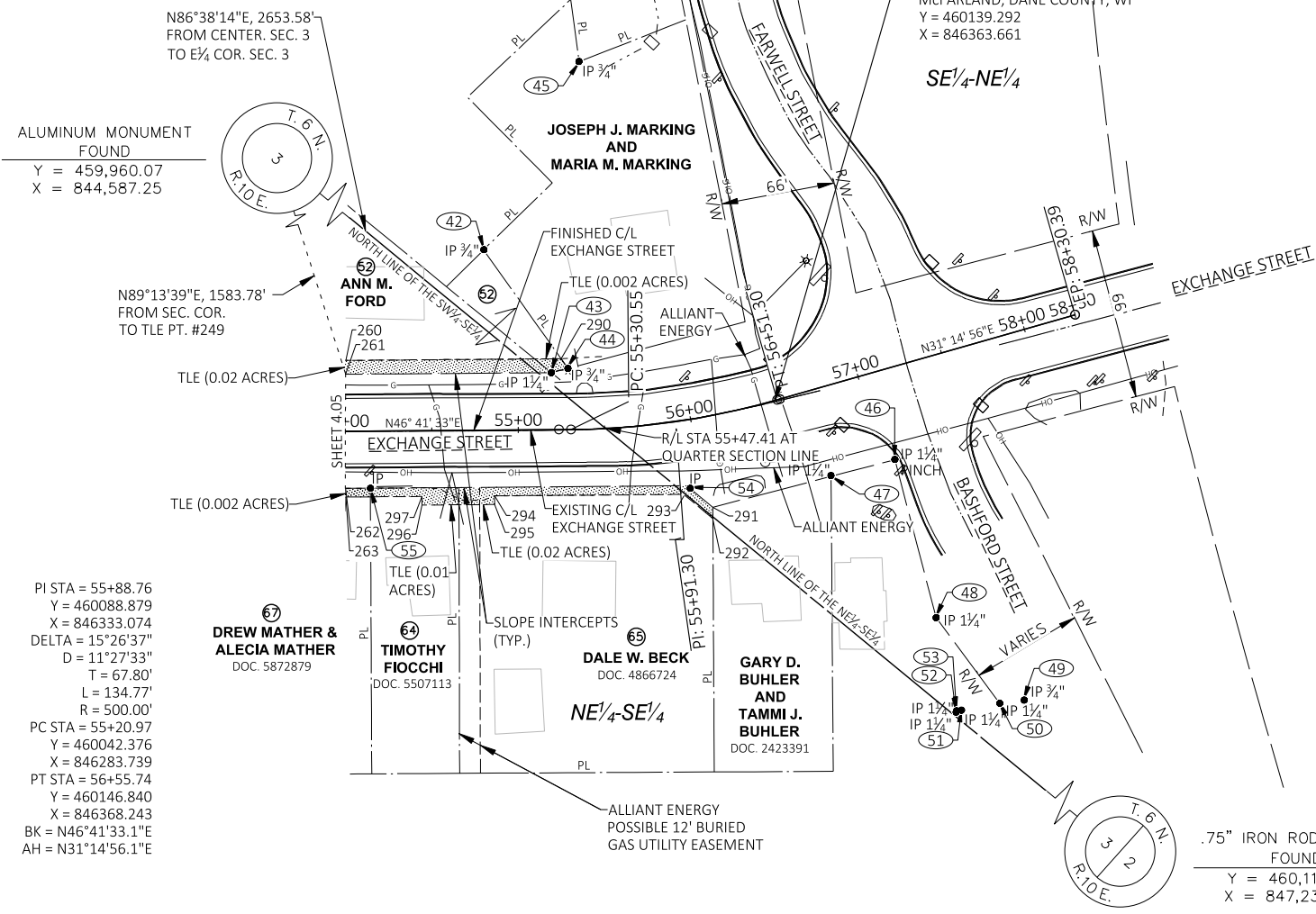
NOTE: EXISTING C/L OF EXCHANGE STREET BASED ON THE LOCATION OF THE EXISTING PAVEMENT.
EXISTING RIGHT-OF-WAY FOR EXCHANGE STREET BASED ON THE LOCATION OF THE C/L OF THE EXISTING PAVEMENT, COUNTY RECORDS, ASSOCIATED FOUND SURVEY MONUMENTATION, AND WS. STATUTE 82.31(2).

REVISION DATE 7-27-2023 10-10-2023	DATE October 10, 2023	SCALE, FEET 0 50 100	HWY: EXCHANGE STREET COUNTY: DANE	STATE R/W PROJECT NUMBER 5685-00-04/05 CONSTRUCTION PROJECT NUMBER	PLAT SHEET 4.05 PS&E SHEET	E
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END RELOCATION ORDER

STA. 56+50.00
 872.60' WEST AND 23.57' NORTH OF
 THE EAST QUARTER CORNER OF
 SECTION 3, T.6N., R.10E., VILLAGE OF
 McFARLAND, DANE COUNTY, WI
 Y = 460139.292
 X = 846363.661



ALUMINUM MONUMENT
 FOUND
 Y = 459,960.07
 X = 844,587.25

N89°13'39"E, 1583.78'
 FROM SEC. COR.
 TO TLE PT. #249

PI STA = 55+88.76
 Y = 460088.879
 X = 846333.074
 DELTA = 15°26'37"
 D = 67.80'
 T = 134.77'
 R = 500.00'
 PC STA = 55+20.97
 Y = 460042.376
 X = 846283.739
 PT STA = 56+55.74
 Y = 460146.840
 X = 846368.243
 BK = N46°41'33.1"E
 AH = N31°14'56.1"E

.75" IRON ROD W/ CAP
 FOUND
 Y = 460,115.73
 X = 847,236.26

FOUND MONUMENTS				
PT#	STATION	OFFSET	Y	X
43	55+19.39	33.00 L	460063.233	846257.840
44	55+29.01	35.40 L	460071.570	846263.201
45	55+42.46	212.70 L	460206.094	846147.319
46	57+07.28	51.83 R	460161.380	846437.687
47	56+69.18	50.66 R	460129.402	846416.925
48	57+05.09	146.40 R	460110.444	846517.403
49	57+40.95	206.17 R	460110.091	846587.101
50	57+26.85	204.06 R	460099.129	846577.986
51	57+04.48	201.78 R	460081.189	846564.427
52	57+01.20	202.01 R	460078.269	846562.932
53	57+01.66	201.42 R	460078.972	846562.663
54	55+93.81	39.43 R	460068.691	846362.242
55	54+14.30	33.04 R	459943.127	846226.555

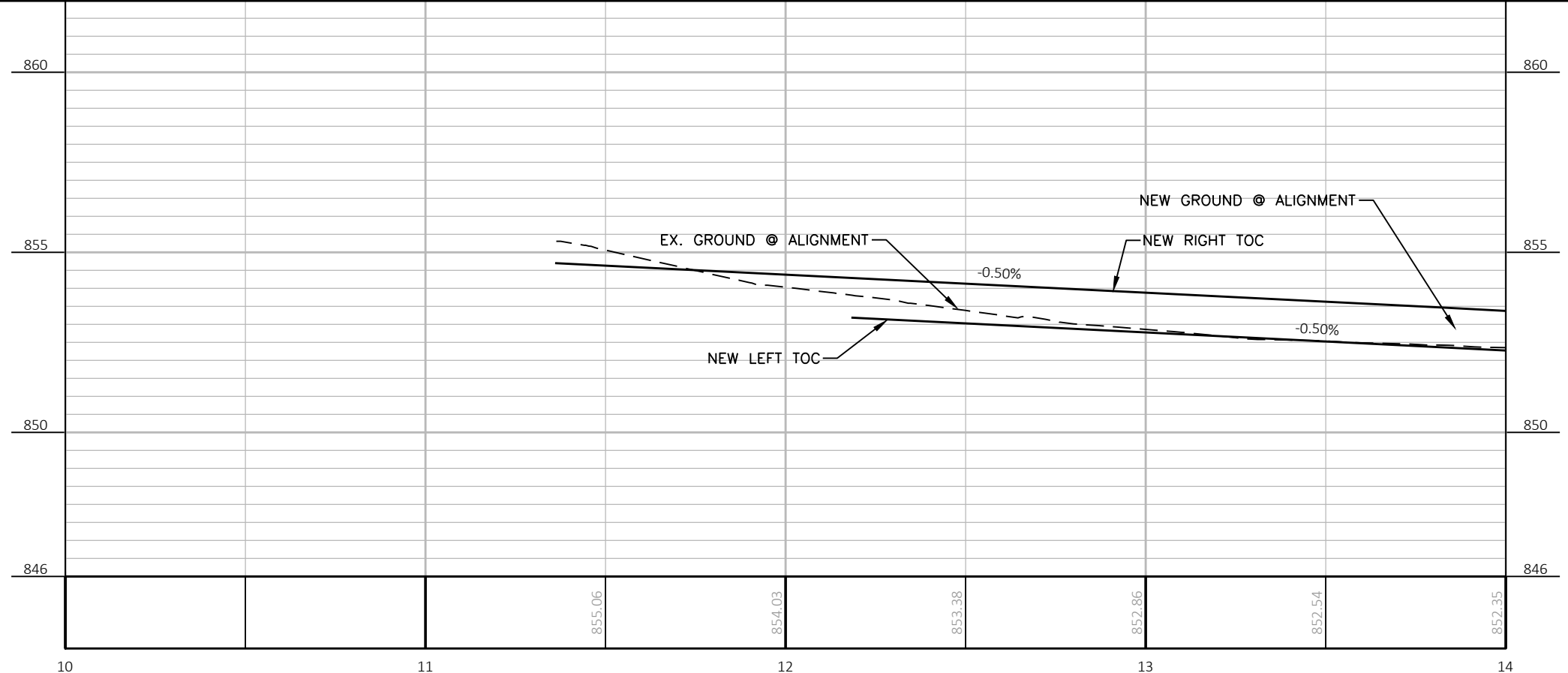
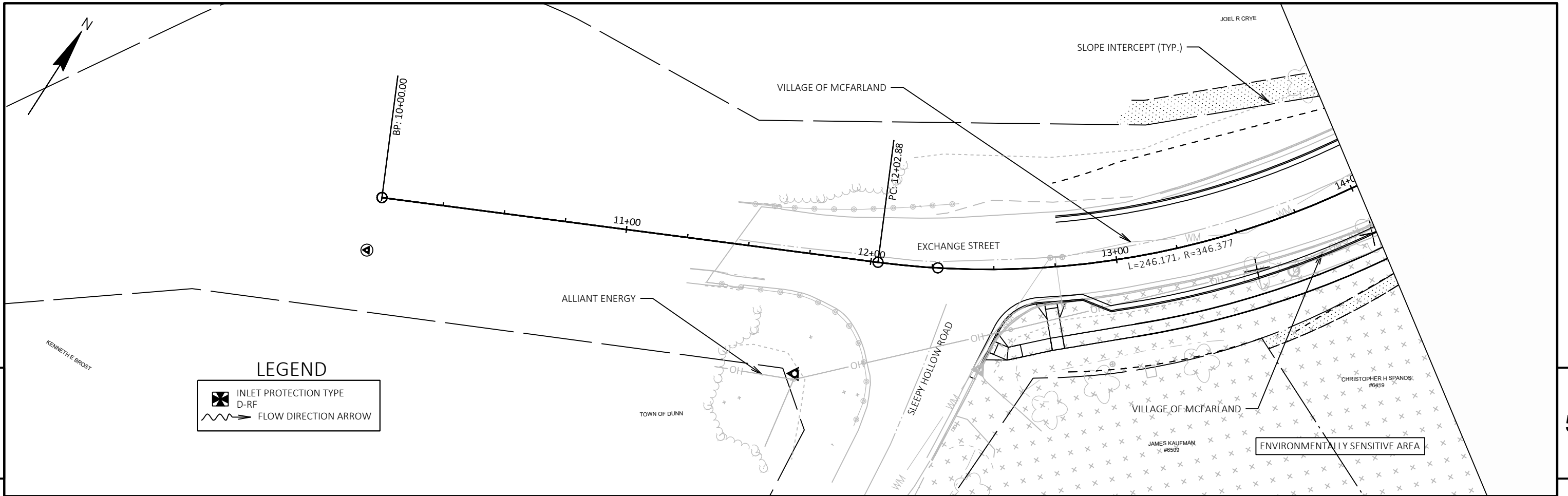
TLE POINTS STATION OFFSET TABLE				
PT#	STATION	OFFSET	Y	X
260	54+00.00	41.00 L	459987.250	846165.404
261	54+00.00	33.00 L	459981.423	846170.886
262	54+00.00	33.00 R	459933.353	846216.111
263	54+00.00	38.00 R	459929.712	846219.538
290	55+25.10	41.00 L	460072.972	846256.518
291	56+03.89	52.34 R	460069.505	846379.253
292	56+02.90	59.00 R	460064.580	846383.869
293	55+90.00	43.00 R	460063.211	846362.406
294	54+86.00	38.00 R	459988.642	846282.175
295	54+86.00	43.00 R	459985.001	846285.601
296	54+44.00	43.00 R	459956.220	846255.010
297	54+44.00	38.00 R	459959.861	846251.584

VILLAGE OF McFARLAND

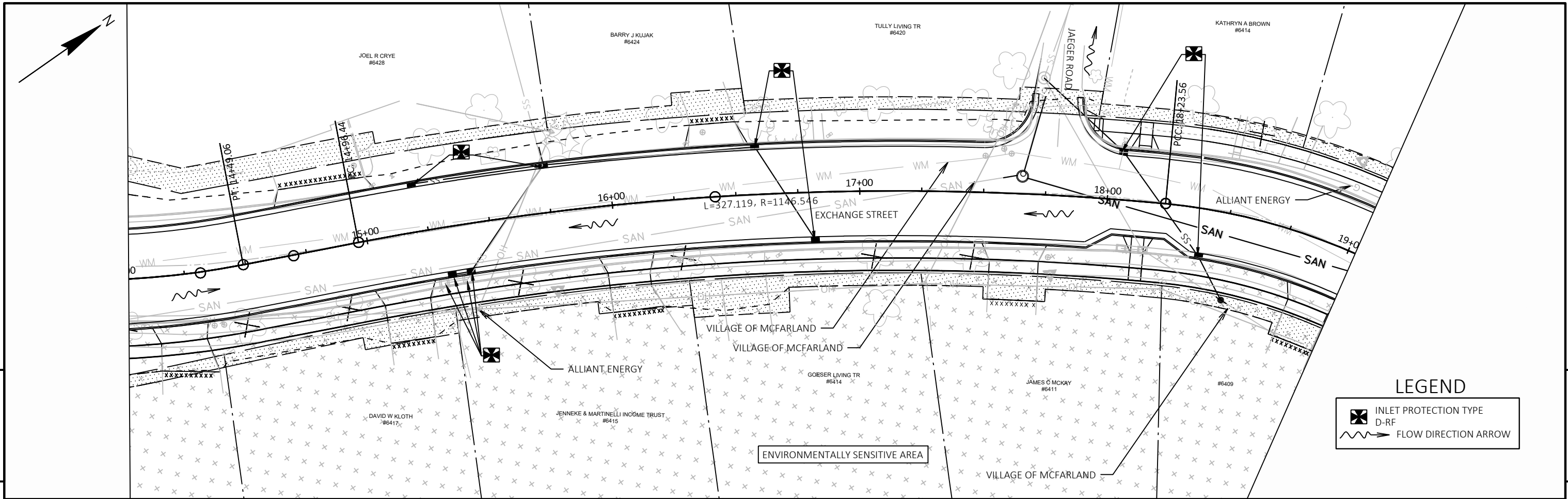
NOTE: OWNER'S NAMES ARE SHOWN FOR REFERENCE PURPOSES ONLY AND ARE SUBJECT TO CHANGE PRIOR TO THE TRANSFER OF LAND INTERESTS TO VILLAGE OF McFARLAND.

NOTE: EXISTING C/L OF EXCHANGE STREET BASED ON THE LOCATION OF THE EXISTING PAVEMENT.
 EXISTING RIGHT-OF-WAY FOR EXCHANGE STREET BASED ON THE LOCATION OF THE C/L OF THE EXISTING PAVEMENT, COUNTY RECORDS, ASSOCIATED FOUND SURVEY MONUMENTATION, AND WS. STATUTE 82.31(2).

REVISION DATE 7-27-2023 10-10-2023 N.C.	DATE October 10, 2023	SCALE, FEET 0 50 100	HWY: EXCHANGE STREET COUNTY: DANE	STATE R/W PROJECT NUMBER 5685-00-04/05 CONSTRUCTION PROJECT NUMBER	PLAT SHEET 4.06 PS&E SHEET	E
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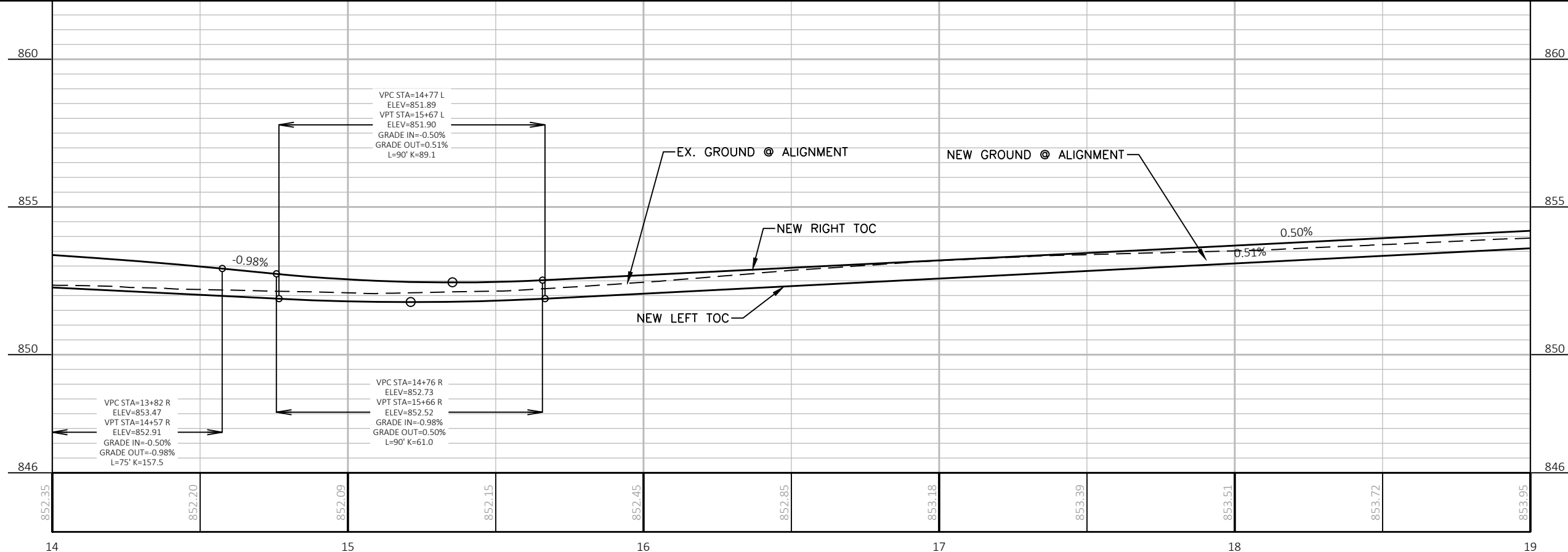


PROJECT NO: 5685-00-05 | HWY: EXCHANGE STREET | COUNTY: DANE | PLAN AND PROFILE: EXCHANGE STREET | SHEET 89 | E

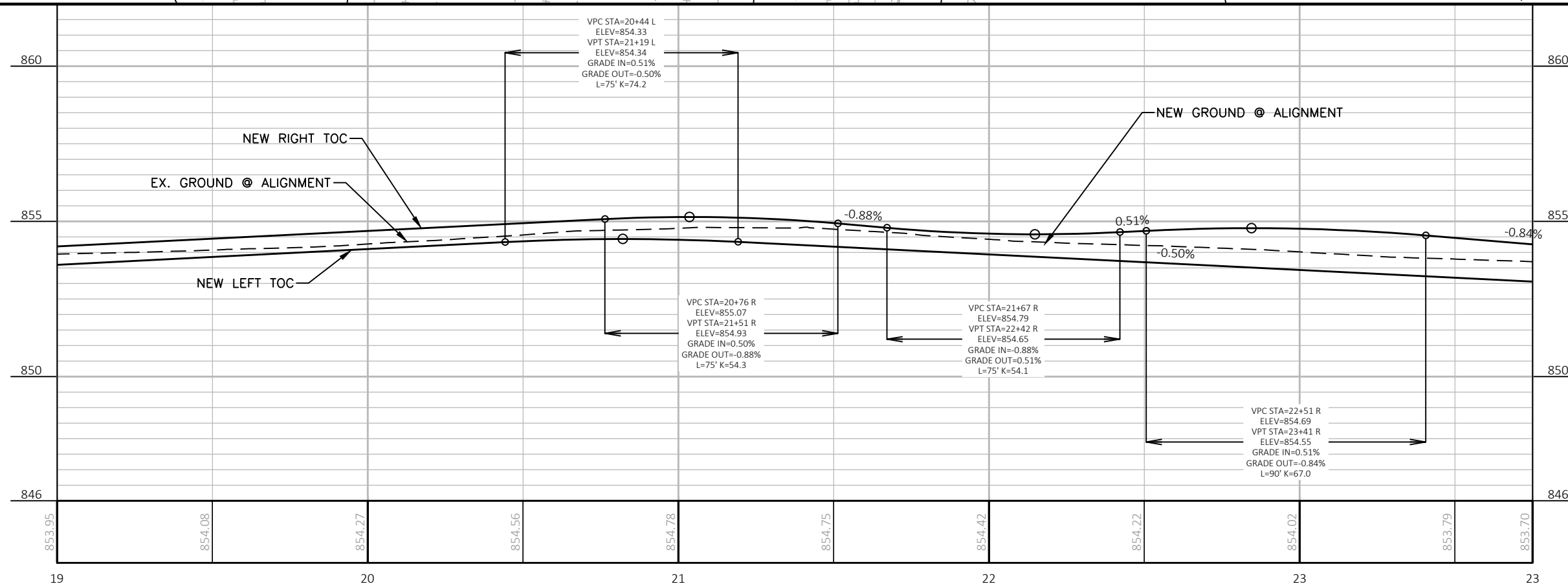
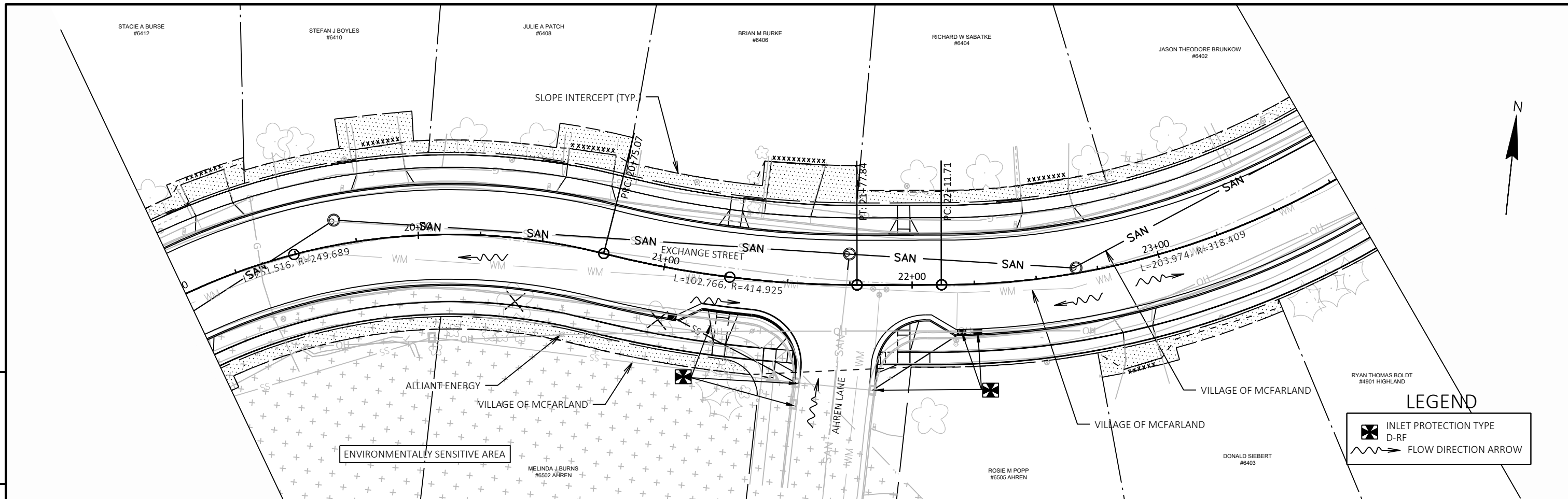


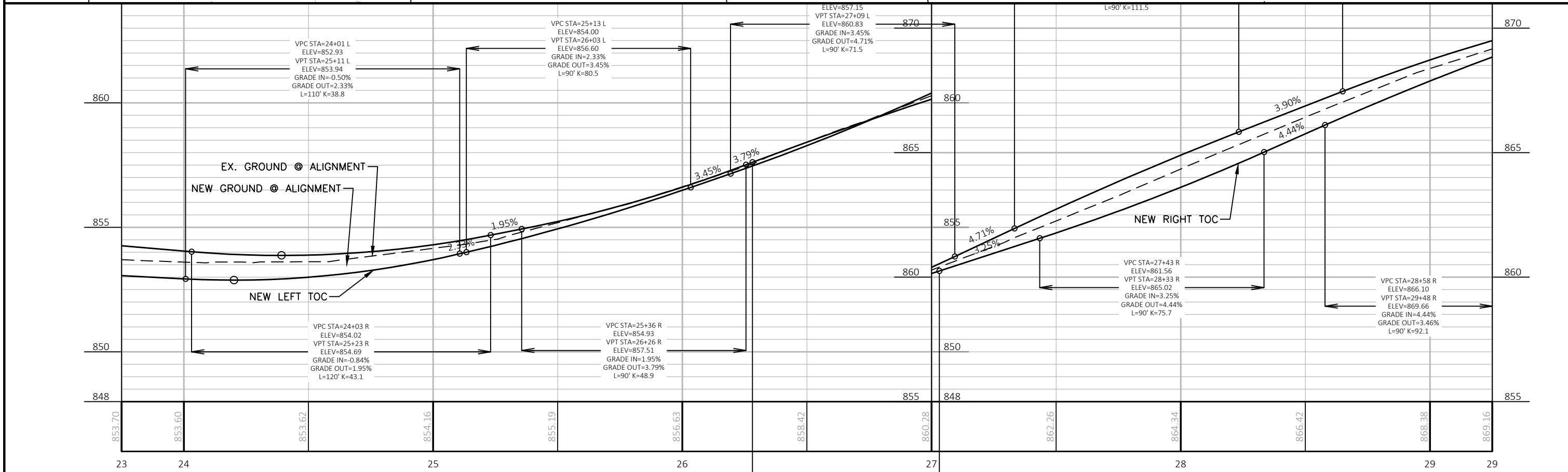
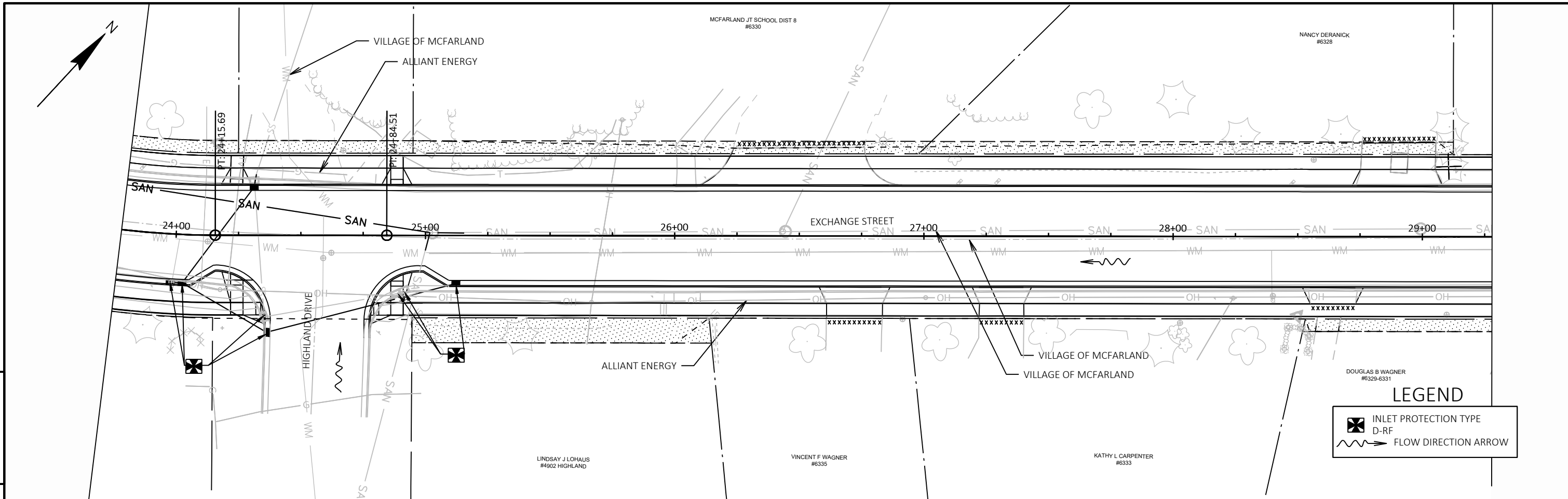
LEGEND

- INLET PROTECTION TYPE D-RF
- FLOW DIRECTION ARROW

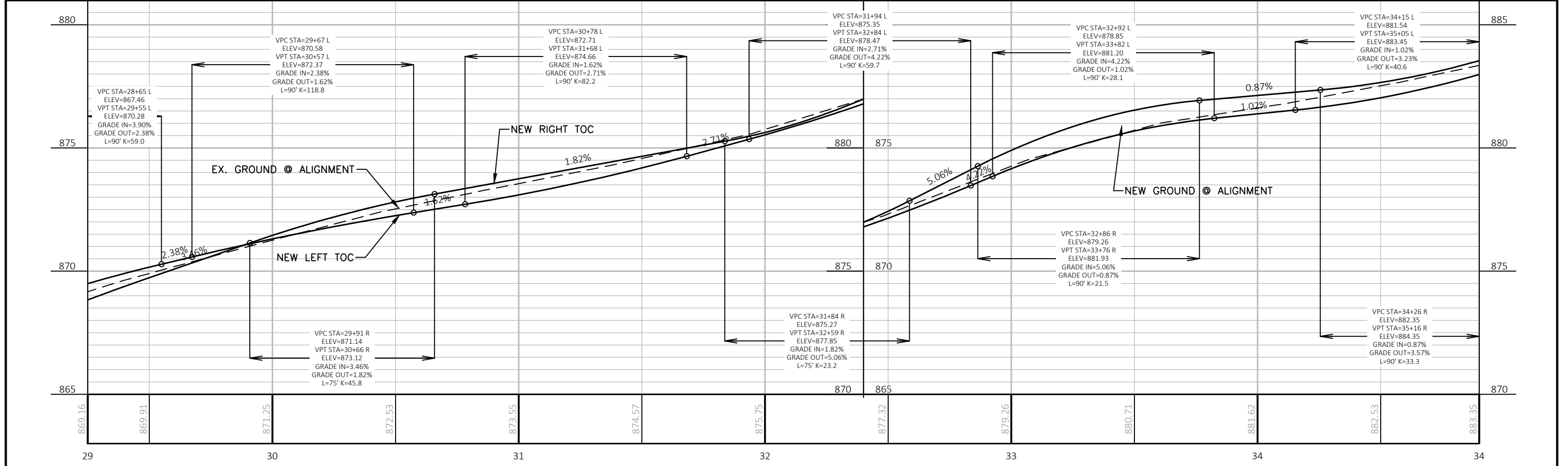
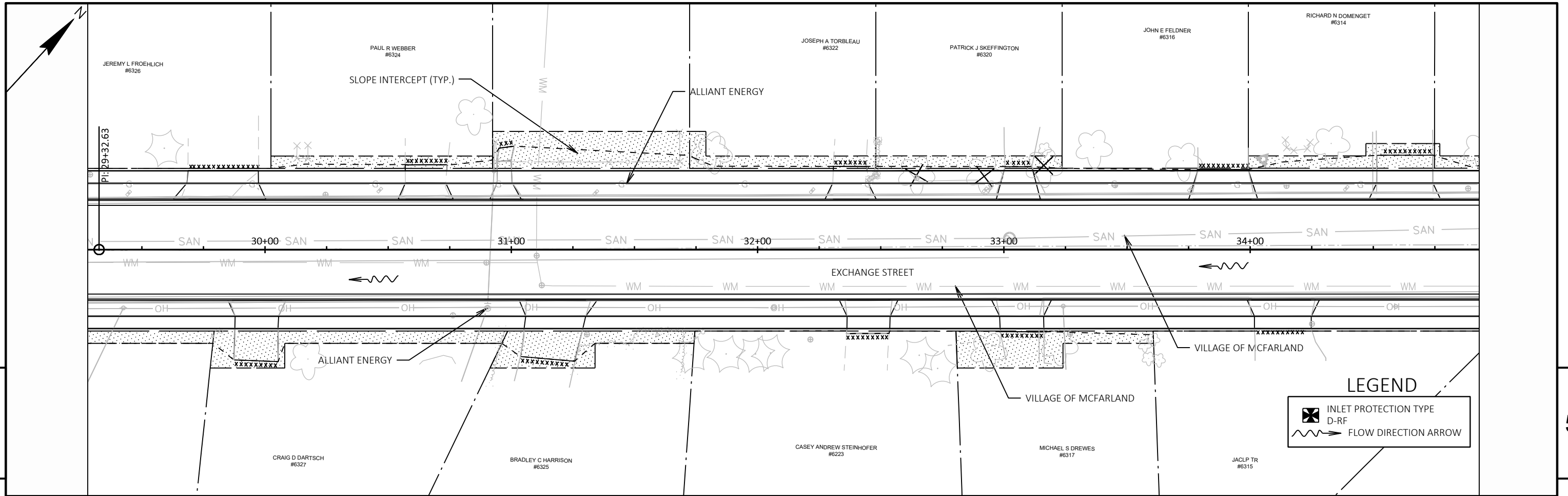


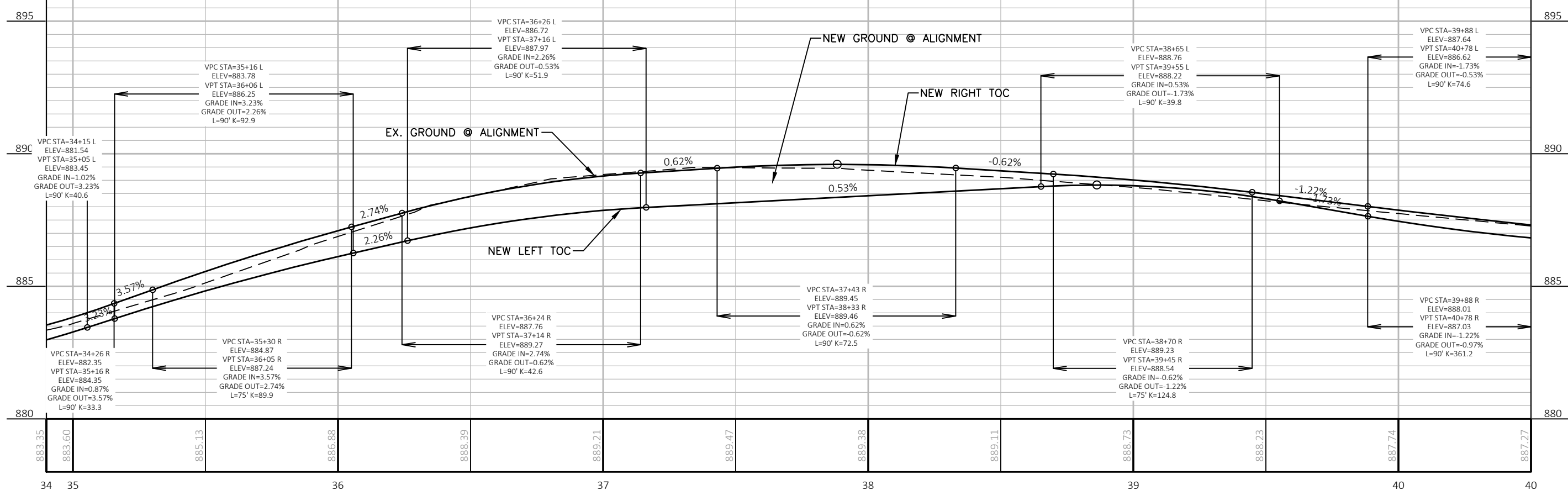
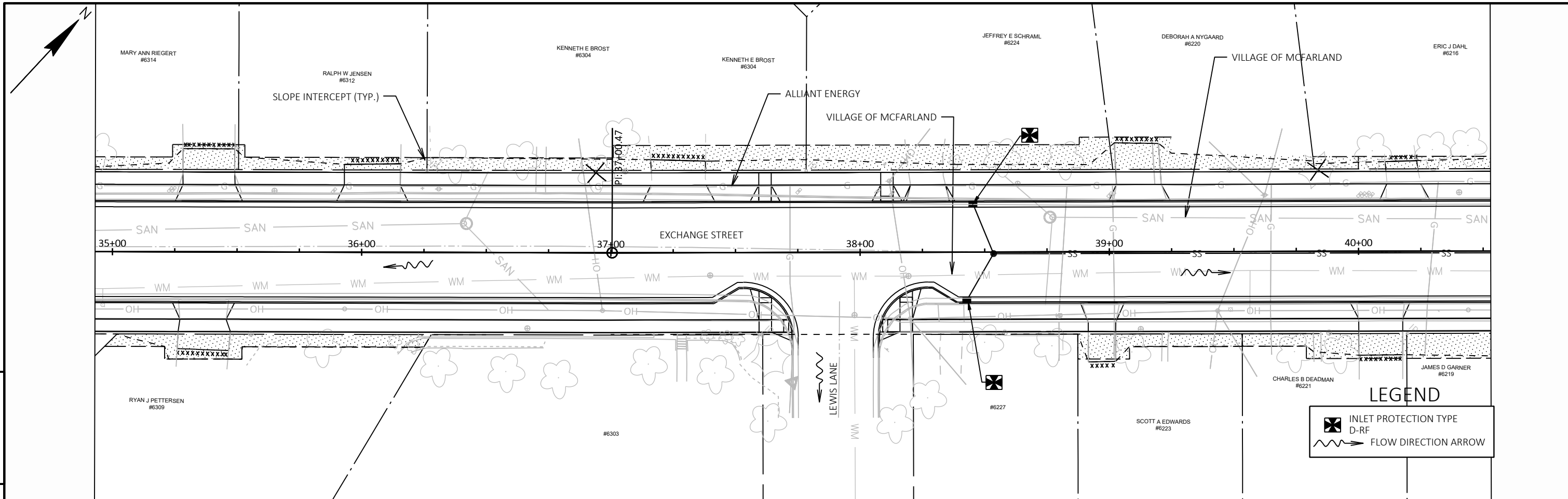
PROJECT NO: 5685-00-05 HWY: EXCHANGE STREET COUNTY: DANE PLAN AND PROFILE: EXCHANGE STREET SHEET 90 E

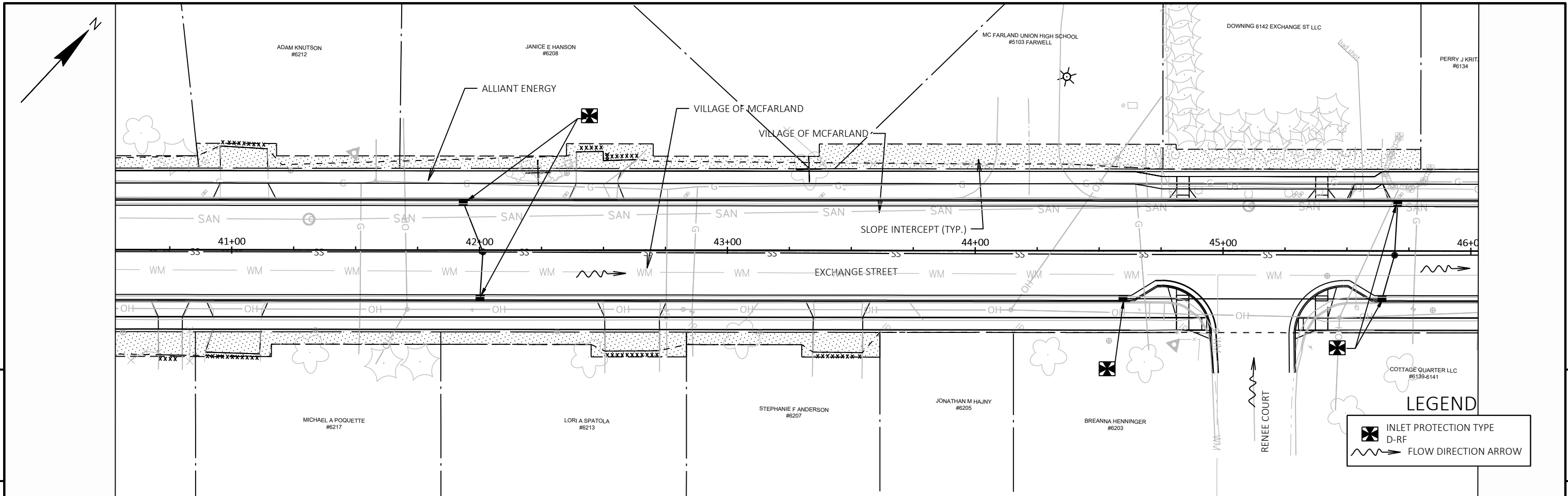




PROJECT NO: 5685-00-05 HWY: EXCHANGE STREET COUNTY: DANE PLAN AND PROFILE: EXCHANGE STREET SHEET 92 E

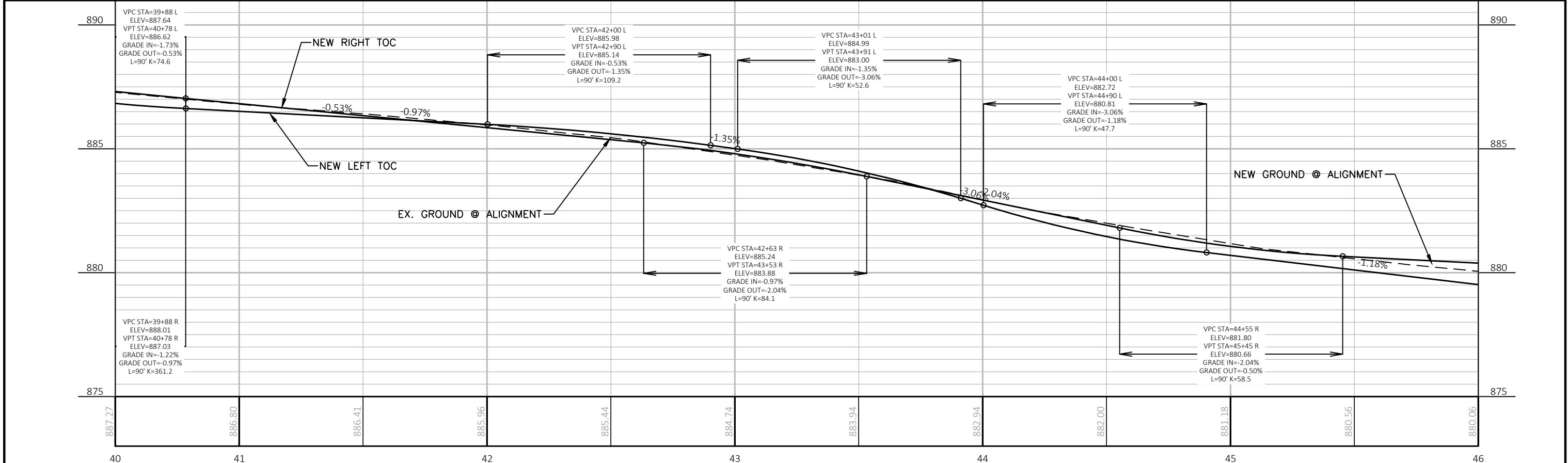


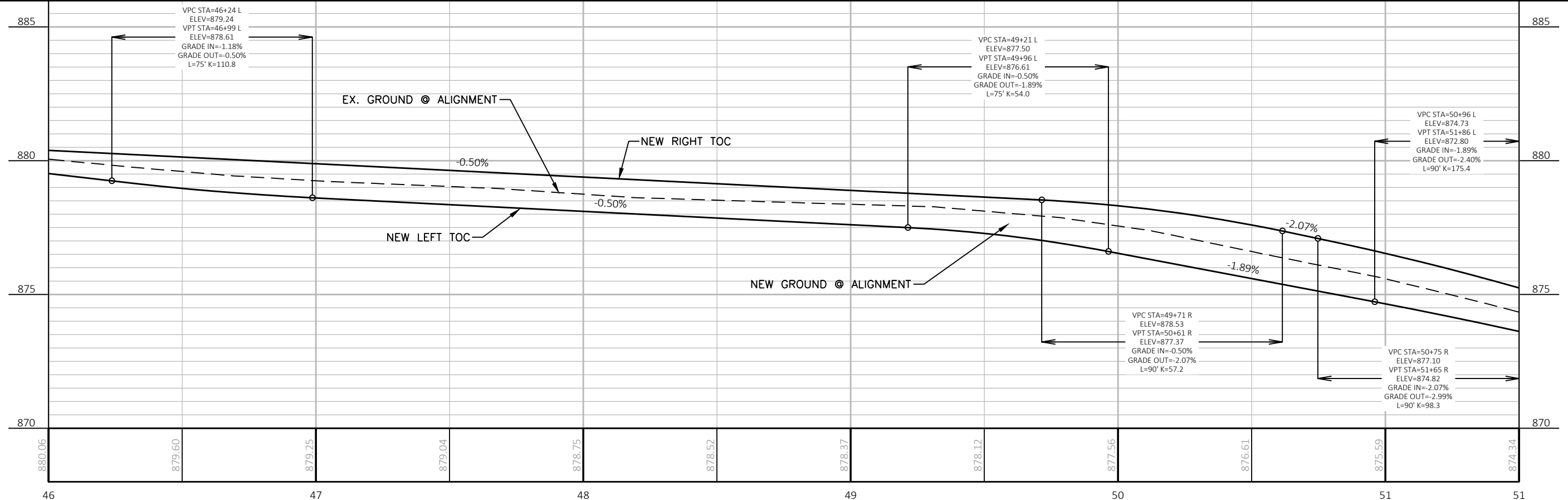
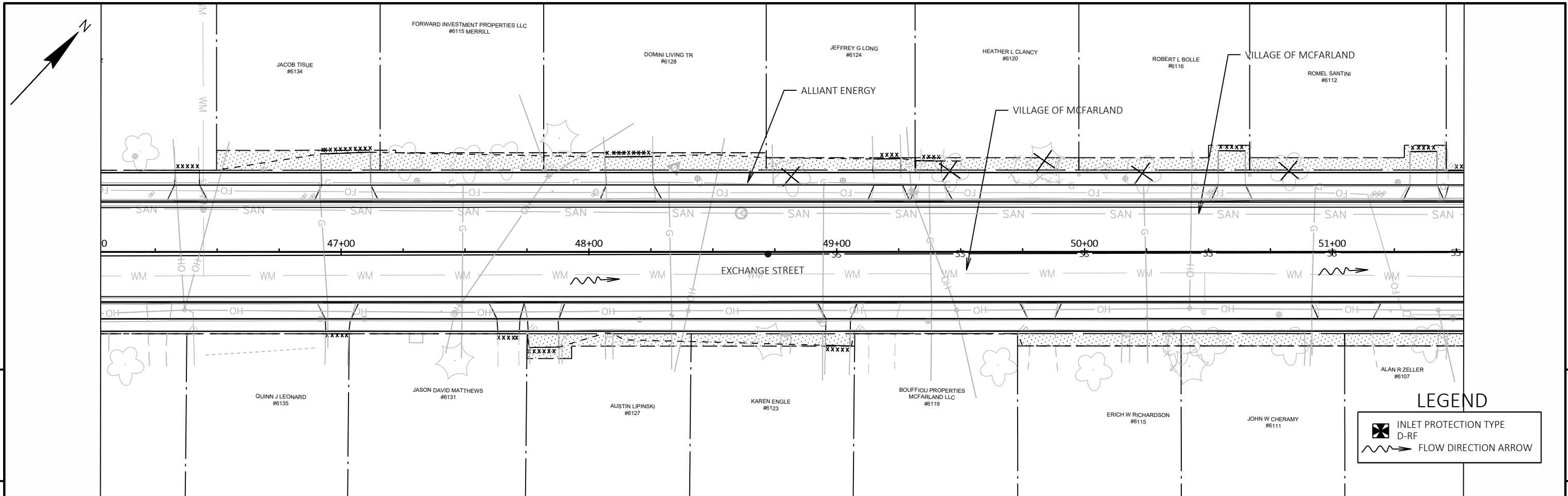




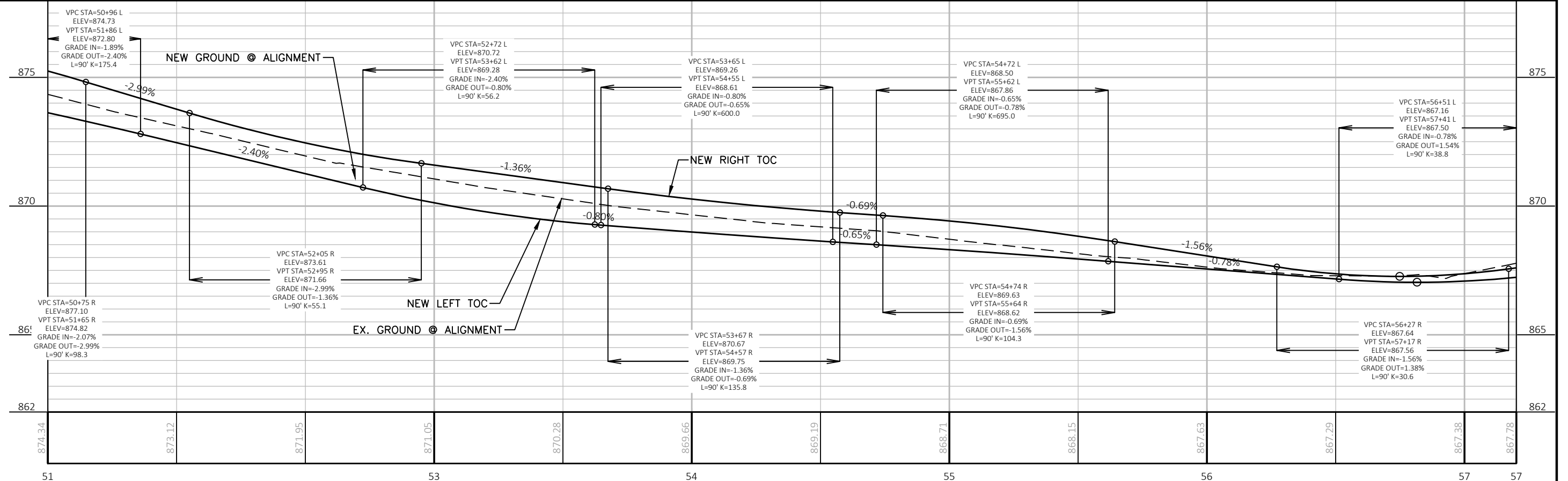
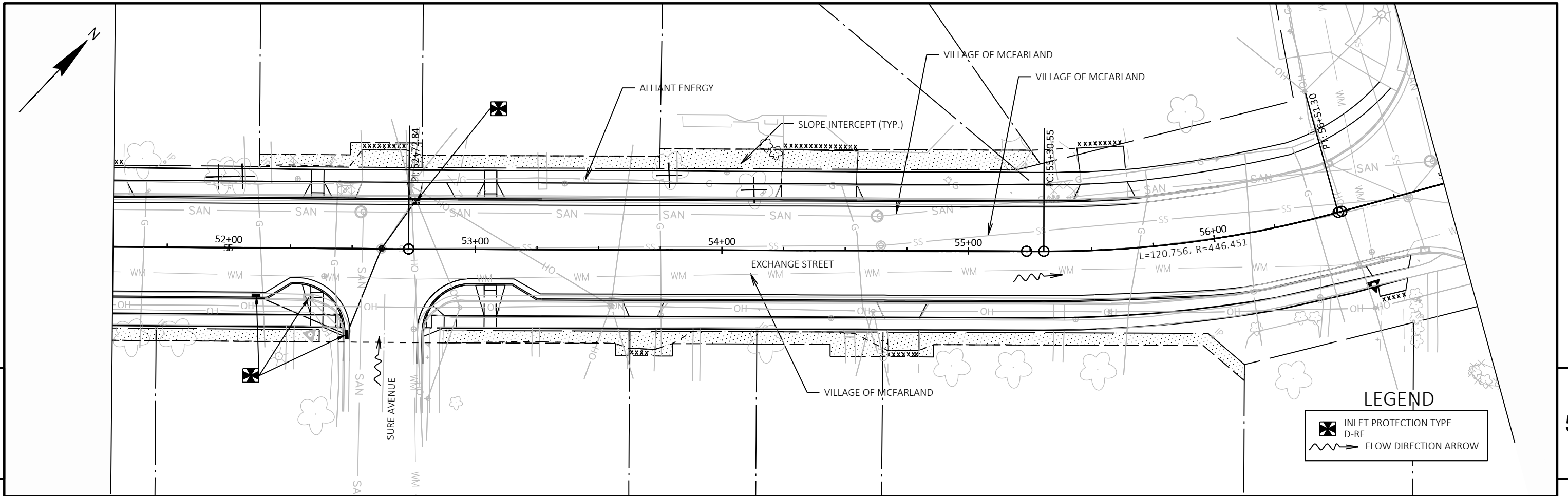
LEGEND

- INLET PROTECTION TYPE D-RF
- FLOW DIRECTION ARROW





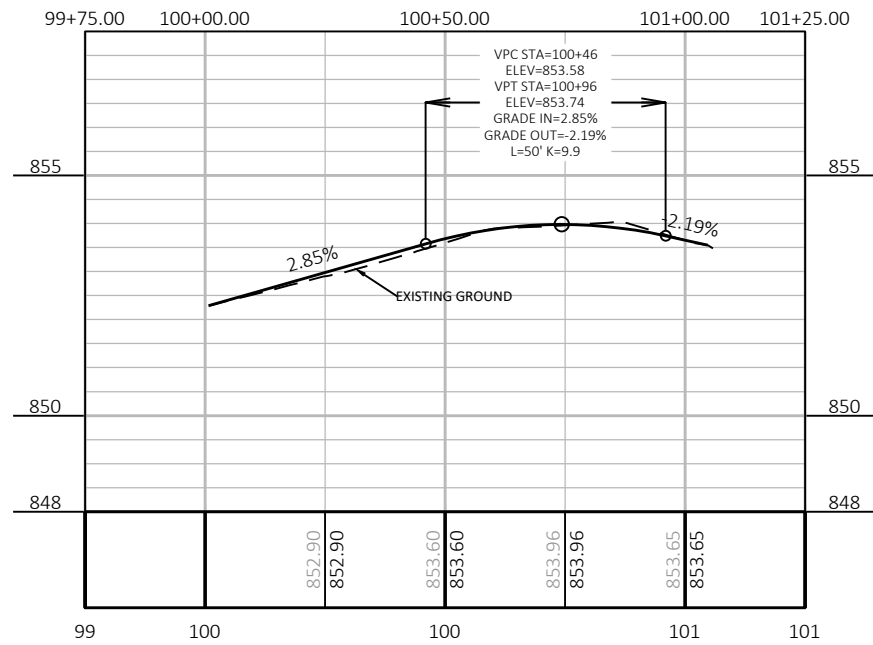
PROJECT NO: 5685-00-05 HWY: EXCHANGE STREET COUNTY: DANE PLAN AND PROFILE: EXCHANGE STREET SHEET 96 E



PROJECT NO: 5685-00-05 HWY: EXCHANGE STREET COUNTY: DANE PLAN AND PROFILE: EXCHANGE STREET SHEET 97 E

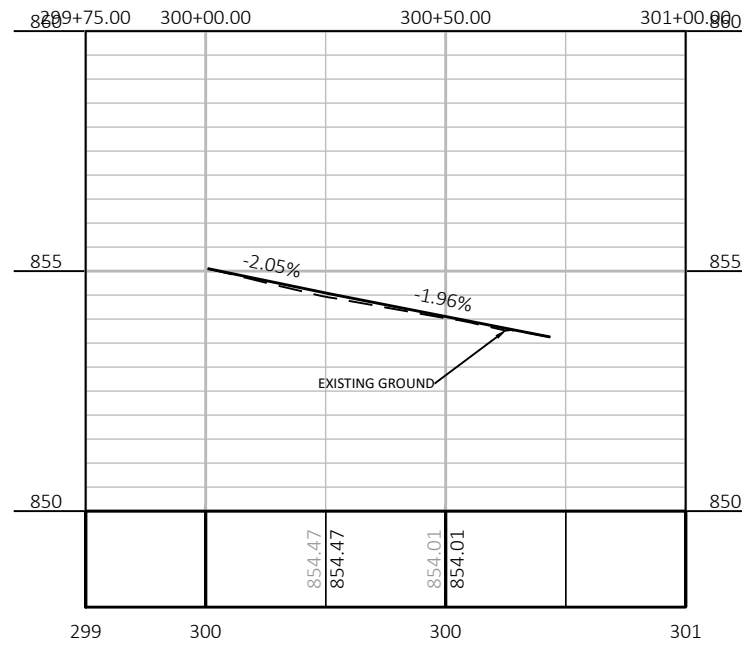
Profile View of SLEEPY HOLLOW ALIGNMENT

V Scale : 4
H Scale : 40
Exaggeration : 10



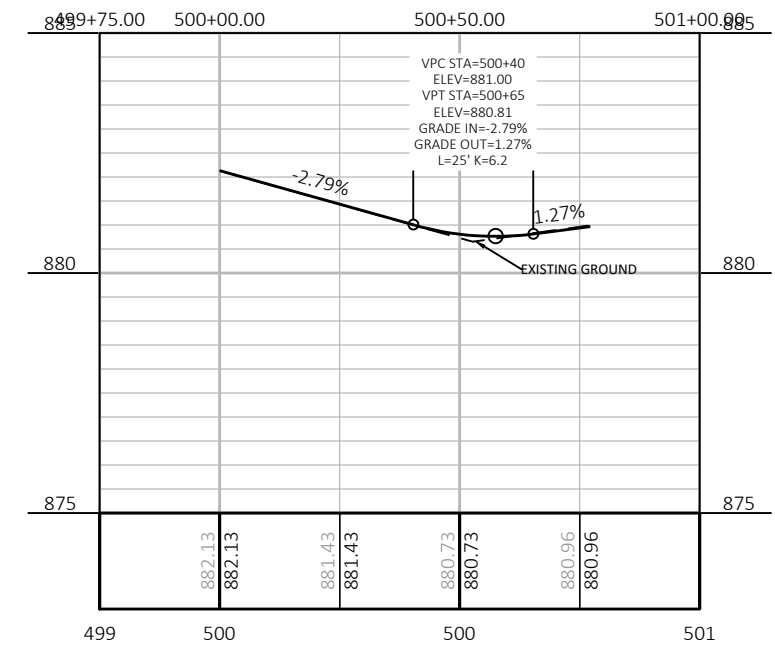
Profile View of HIGHLAND ALIGNMENT

V Scale : 4
H Scale : 40
Exaggeration : 10



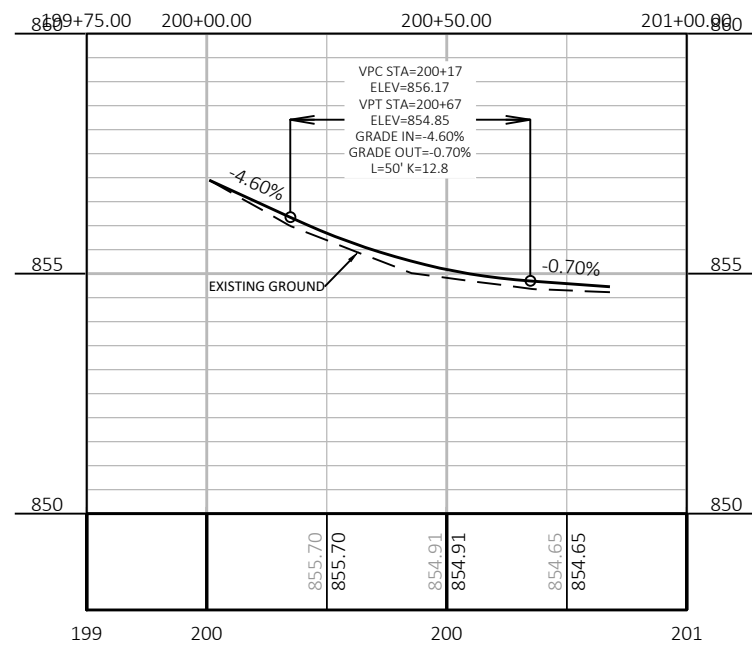
Profile View of RENEE ALIGNMENT

V Scale : 4
H Scale : 40
Exaggeration : 10



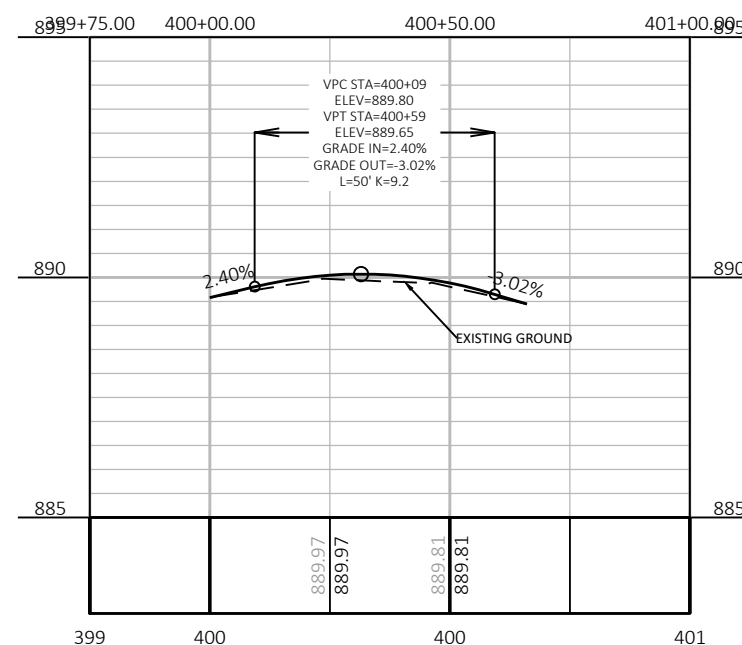
Profile View of AHREN ALIGNMENT

V Scale : 4
H Scale : 40
Exaggeration : 10



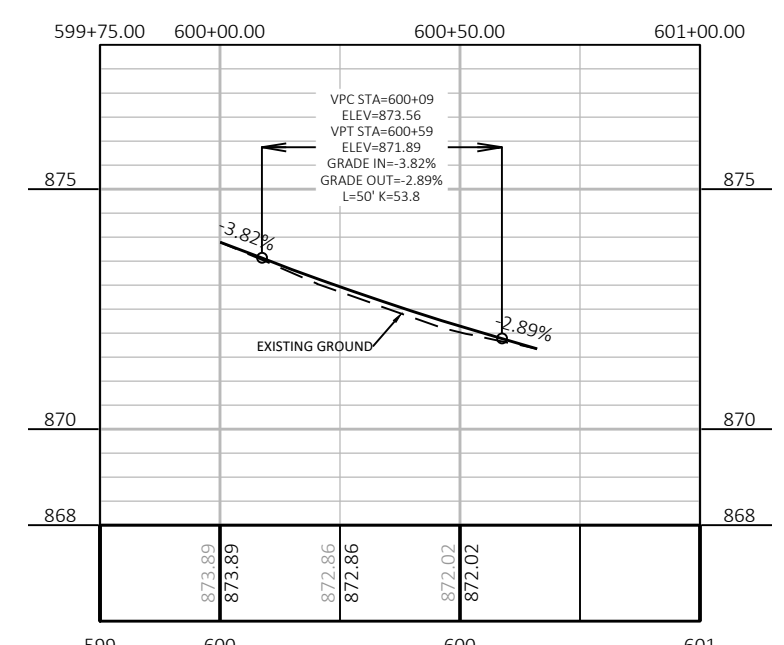
Profile View of LEWIS ALIGNMENT

V Scale : 4
H Scale : 40
Exaggeration : 10



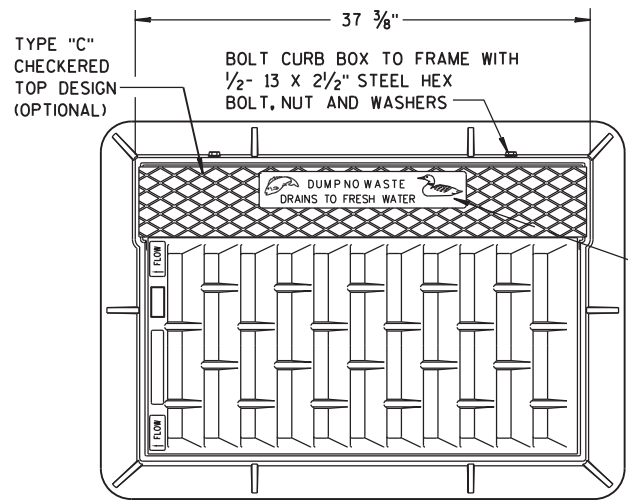
Profile View of SURE ALIGNMENT

V Scale : 4
H Scale : 40
Exaggeration : 10

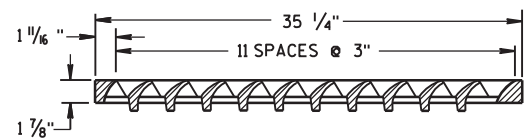
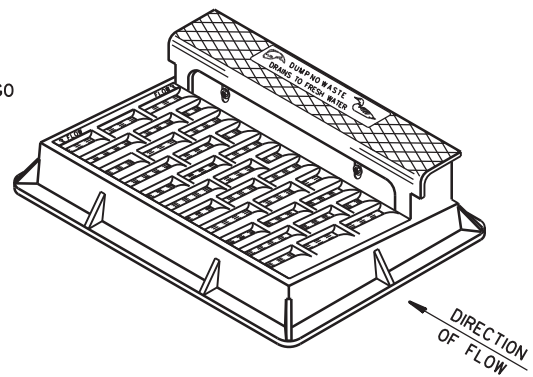


Standard Detail Drawing List

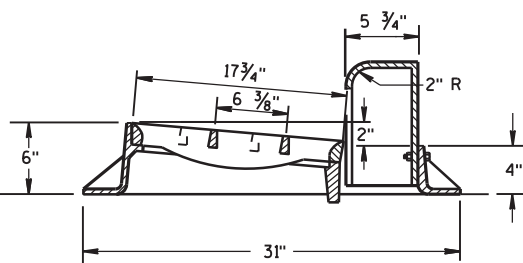
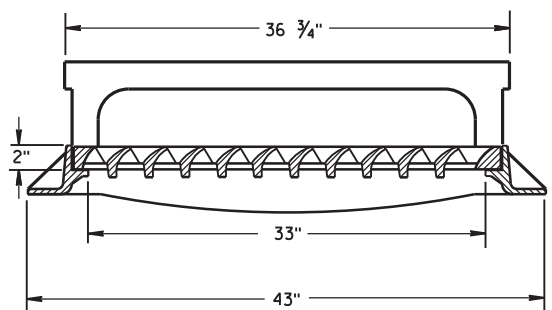
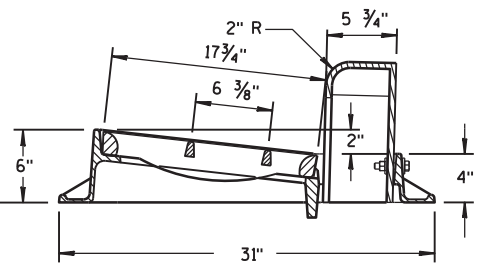
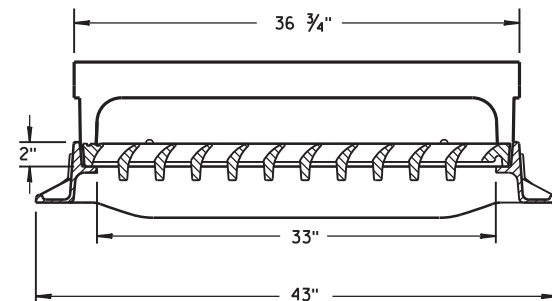
08A05-19A	INLET COVERS TYPE A, H, A-S, H-S & Z
08A05-19C	INLET COVERS TYPE F, HM, HM-S, S, T, V, HM-GJ, & HM-GJ-S
08A05-19D	INLET COVER TYPE BW, MANHOLE COVERS, TYPE K, J, J-S, L & M
08A08-02	CATCH BASINS 3-FT, 4-FT, 5-FT AND 6-FT DIAMETER
08A09-02	CATCH BASINS 2X3-FT AND 2.5X3-FT
08B09-03	MANHOLES 3-FT, 4-FT, 5-FT, 6-FT, 7-FT, 8-FT, 9-FT, 10-FT DIAMETER
08C07-02	INLETS 2X2-FT, 2X2.5-FT, 2X3-FT AND 2.5X3-FT
08D01-23A	CONCRETE CURB & GUTTER
08D01-23B	CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS
08D05-20A	CURB RAMPS TYPES 1 AND 1-A
08D05-20B	CURB RAMPS TYPES 2 AND 3
08D05-20C	CURB RAMPS TYPES 4A AND 4A1
08D05-20D	CURB RAMPS TYPE 4B AND 4B1
08D05-20E	CURB RAMPS TYPES 5, 6, 7A, 7B & 8
08D05-20F	CURB RAMPS RADIAL DETECTABLE WARNING FIELD APPLICATIONS
08D05-20G	CURB RAMPS RECTANGULAR AND RADIAL DETECTABLE WARNING PLATES
08D18-03	DRIVEWAY AND SIDEWALK RAMPS TYPES X & Y
08D19-03	DRIVEWAY AND SIDEWALK RAMPS TYPE Z
08E09-06	SILT FENCE
08E10-02	INLET PROTECTION TYPE A, B, C AND D
08E14-01	TRACKING PAD
09B02-10	CONDUIT
09B16-02	PULL BOX NON-CONDUCTIVE
13C19-03	HMA LONGITUDINAL JOINTS
15C02-09A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-09B	BARRICADES AND SIGNS FOR VARIOUS CLOSURES
15C03-05	BARRICADES AND SIGNS FOR SIDEROAD CLOSURES
15C07-15A	PAVEMENT MARKING SYMBOLS
15C07-15C	PAVEMENT MARKING ARROWS
15C07-15E	PAVEMENT MARKING FOR BIKE LANES
15C08-23A	PERMANENT LONGITUDINAL PAVEMENT MARKINGS
15C29-08A	BICYCLE LANE MARKING
15C29-08B	PAVEMENT MARKING FOR SHARED LANE 35 MPH OR LESS
15C33-04	STOP LINE AND CROSSWALK PAVEMENT MARKING
15D30-09A	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09B	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09C	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09D	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09E	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09F	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09G	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09H	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09I	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09J	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09K	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09L	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION



**NOTE:
GRATE IS REVERSIBLE.**

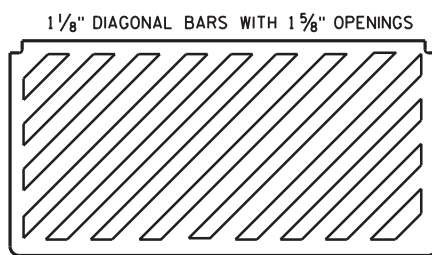


NOTE: CURB BOX HEIGHT ADJUSTABLE 6" TO 9"



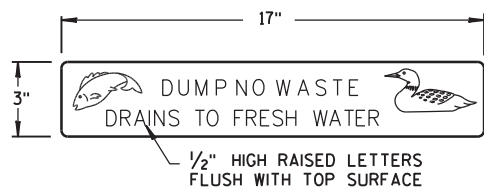
TYPE "H"

NOTE: EITHER CASTING IS ACCEPTABLE

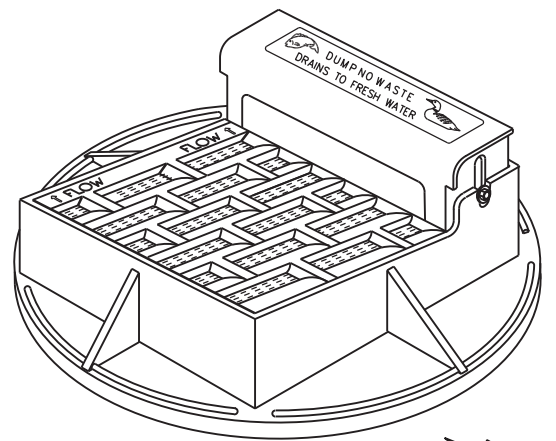


**SPECIAL GRATE FOR
TYPE "H" COVER**

(MEASURES 35 1/4" X 17 3/4" X 2")
(NOTED AS TYPE H-S ON DRAINAGE TABLE)

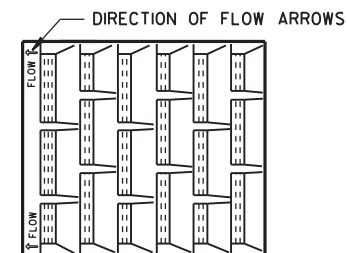


LOGO DETAIL

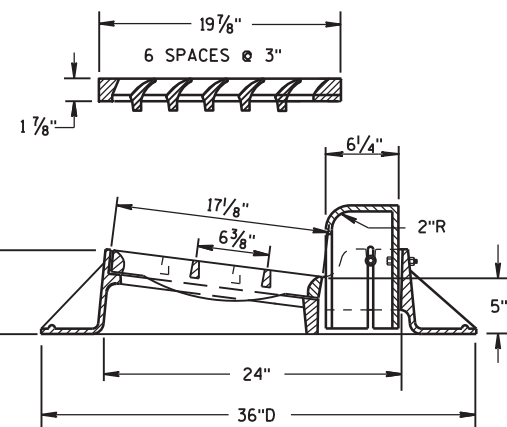
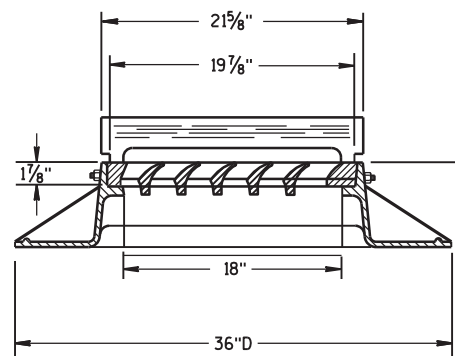


NOTE: CURB BOX ADJUSTABLE 4" TO 9"

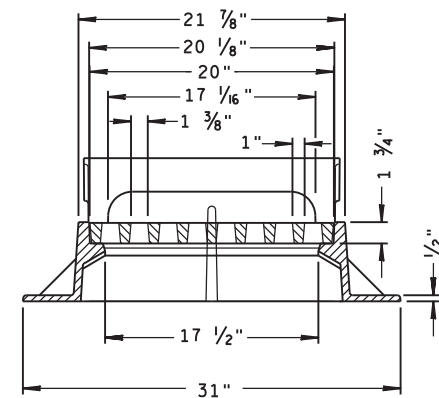
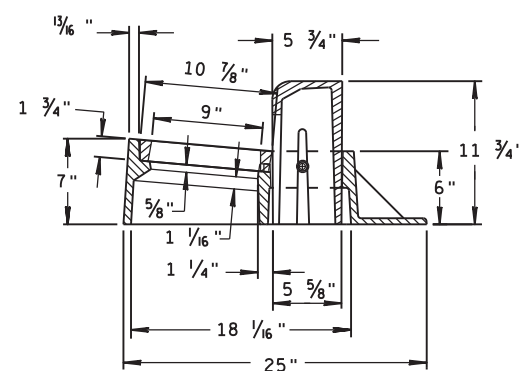
**NOTE:
GRATE IS REVERSIBLE.**



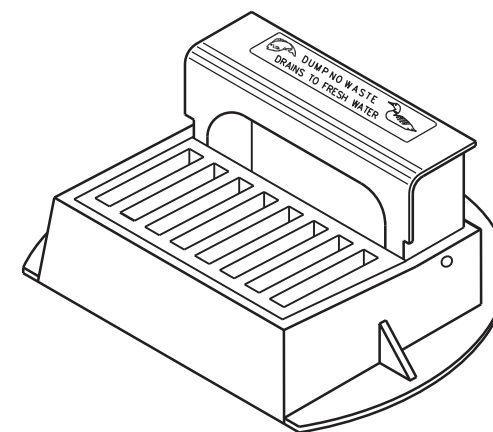
**SPECIAL GRATE FOR
TYPE "A" COVER**
(MEASURES 19 3/4" X 17" X 1 1/8")
(NOTED AS TYPE A-S ON DRAINAGE TABLE)



TYPE "A"



TYPE "Z"

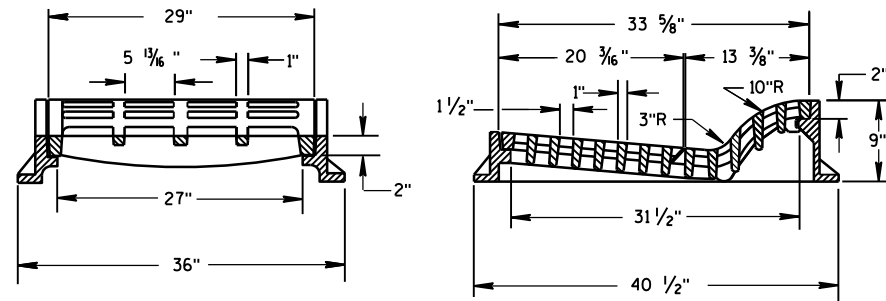
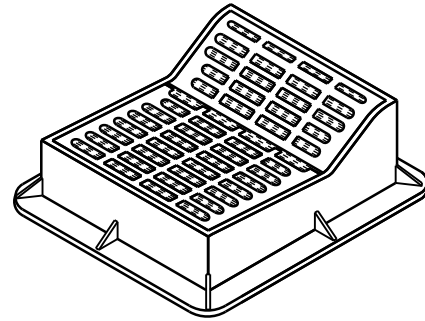


**INLET COVERS
TYPE A, H, A-S, H-S & Z**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
11-27-13
DATE
FHWA

/s/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT
ENGINEER



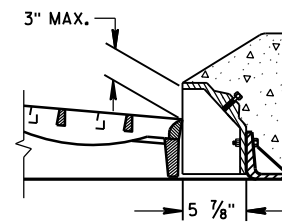
TYPE "F"

USE WITH TYPES A & D CONCRETE CURB & GUTTER, 36 INCH.

GENERAL NOTES

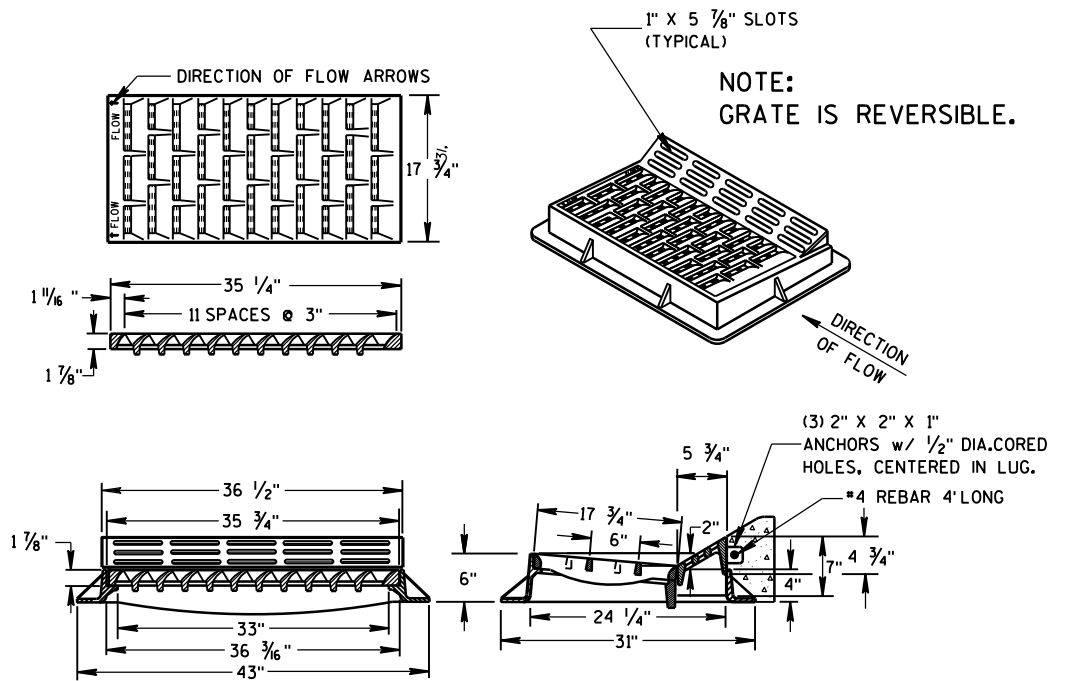
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

DETAIL DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR INLET COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.



ALTERNATIVE CURB BOX FOR TYPE "HM" COVER

USE WITH TYPES G & J CONCRETE CURB & GUTTER, 30 INCH NOTED AS TYPE HM-GJ ON DRAINAGE TABLE

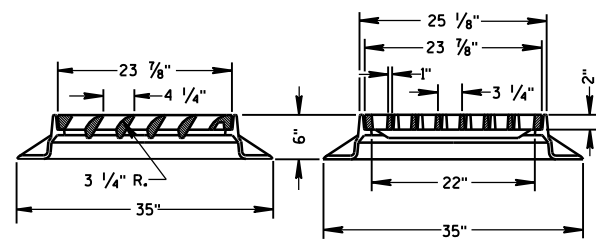
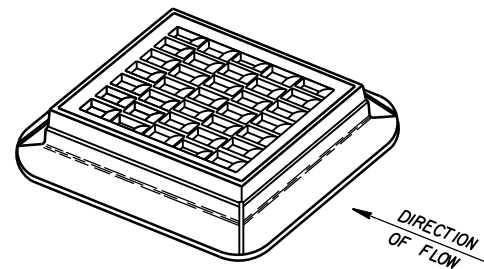


TYPE "HM"

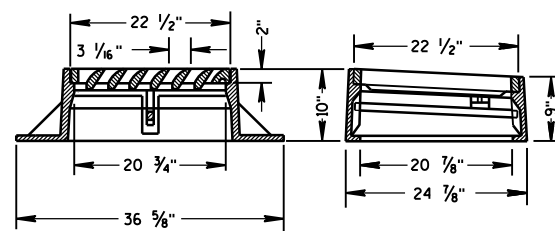
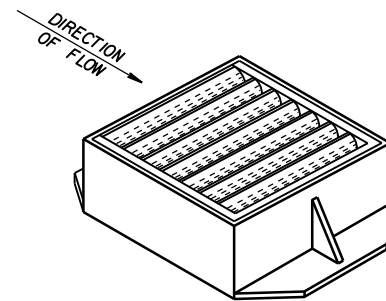
USE WITH TYPES A & D CONCRETE CURB & GUTTER, 36 INCH.

NOTE: SPECIAL GRATE FOR THE TYPE "H" COVER MAY ALSO BE USED FOR THE TYPE "HM" COVER NOTED AS TYPE HM-S ON DRAINAGE TABLE

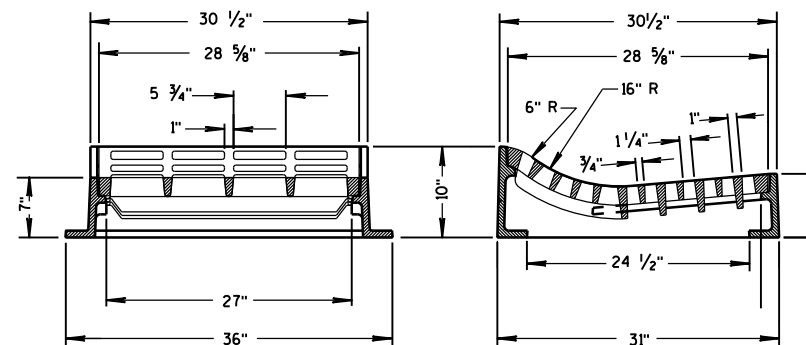
NOTE: SPECIAL GRATE FOR THE TYPE "H" COVER MAY ALSO BE USED FOR THE TYPE "HM-GJ" COVER NOTED AS TYPE HM-GJ-S ON DRAINAGE TABLE



TYPE "S"

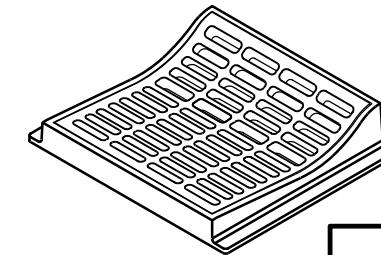


TYPE "V"



TYPE "T"

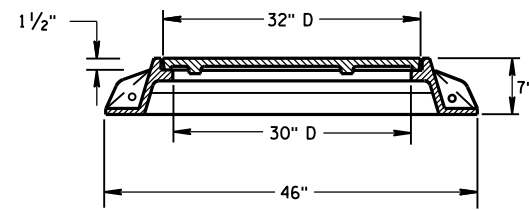
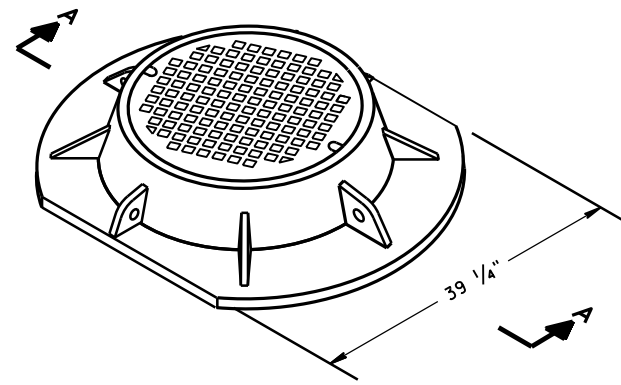
USE WITH TYPES R & T CONCRETE CURB & GUTTER, 36 INCH.



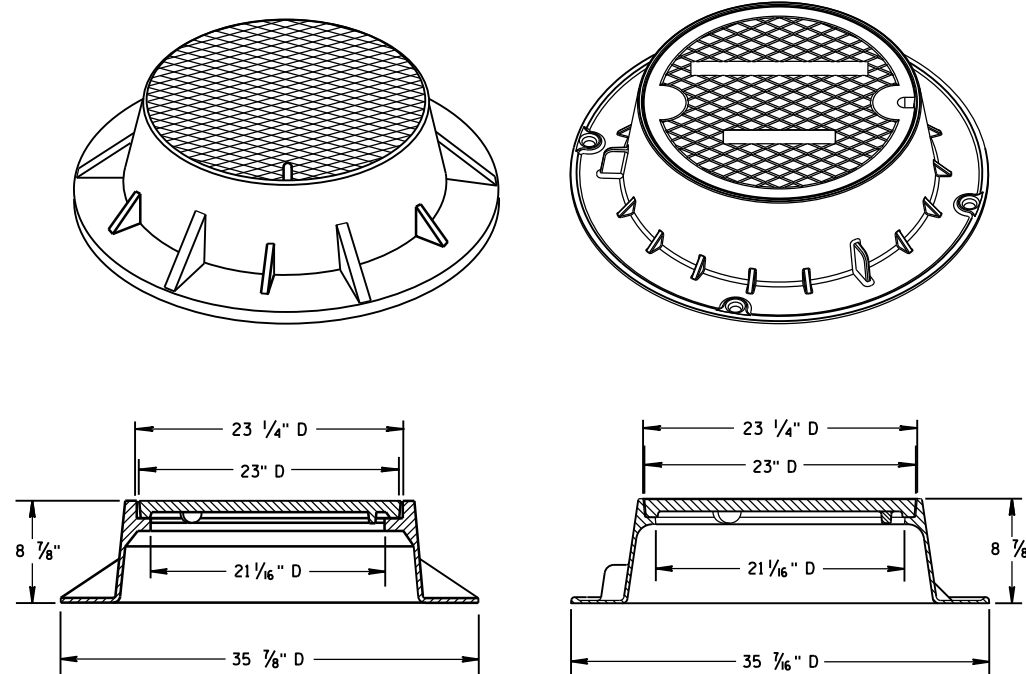
INLET COVERS
TYPE F, HM, HM-S, S, T, V,
HM-GJ, & HM-GJ-S

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
11/27/2013 DATE /s/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA

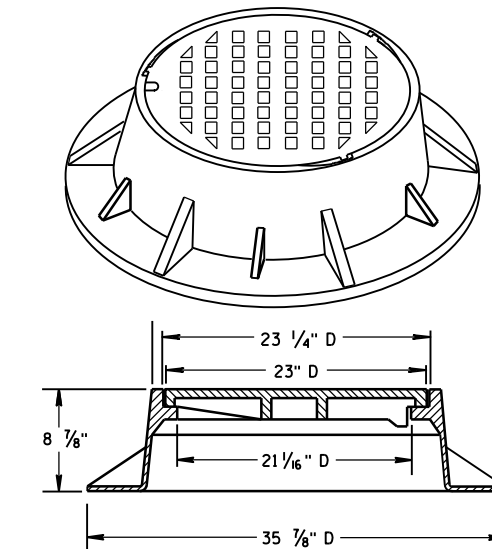
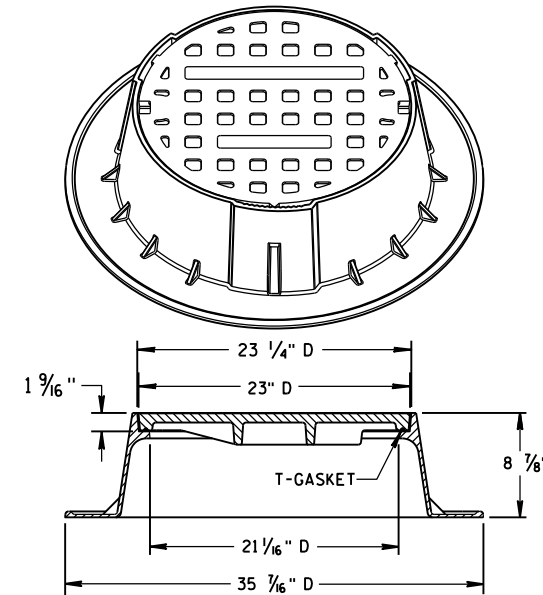


SECTION A-A
TYPE "K"



TYPE "J"

NOTE: EITHER CASTING IS ACCEPTABLE



TYPE "J" SPECIAL

TYPE "B" NON-ROCKING SELF-SEAL LID
(NOTED AS TYPE J-S ON THE DRAINAGE TABLE)

NOTE: EITHER CASTING IS ACCEPTABLE

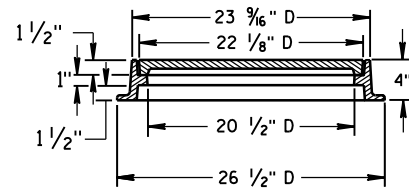
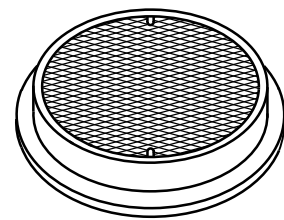
GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

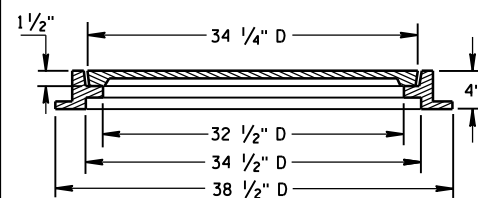
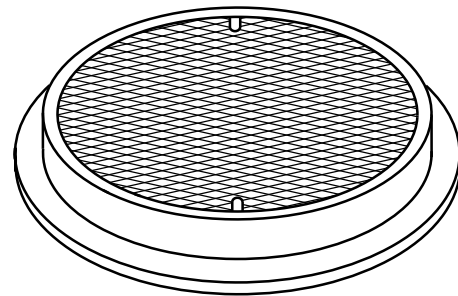
DETAIL DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR MANHOLE COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ROUND FRAMES AND COVERS SHALL HAVE CONTINUOUSLY MACHINED BEARING SURFACES TO PREVENT ROCKING AND RATTLING.

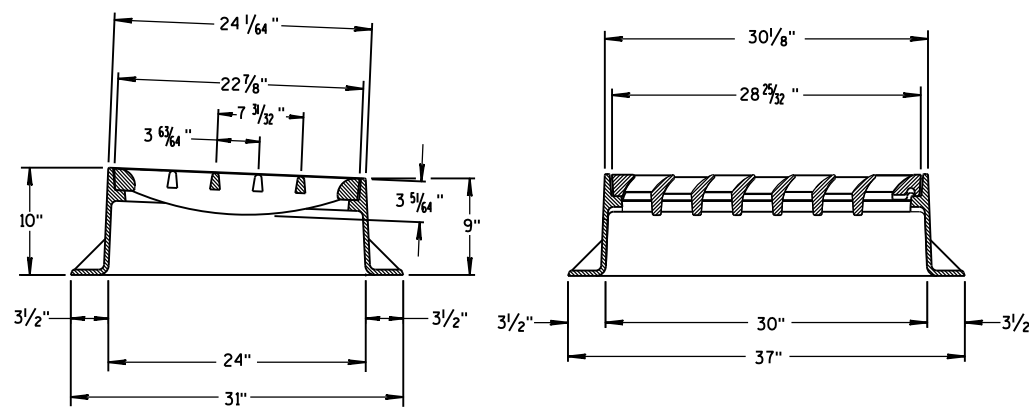
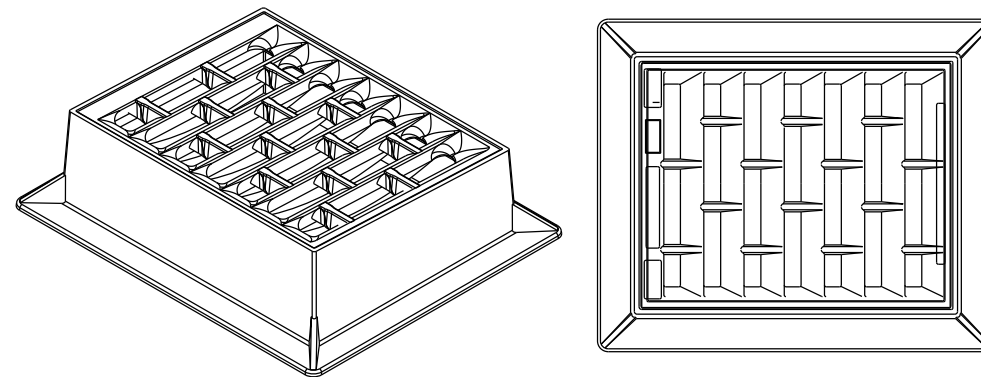
6



TYPE "L"



TYPE "M"



INLET COVER TYPE "BW"

6

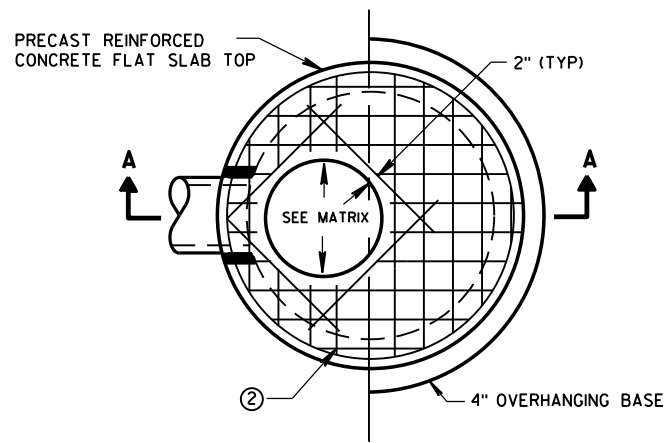
INLET COVER TYPE BW
MANHOLE COVERS, TYPE K,
J, J-S, L & M

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

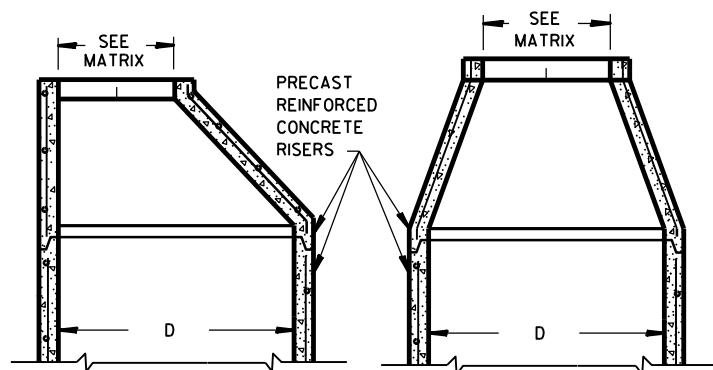
APPROVED
11/27/2013 DATE /S/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT
ENGINEER
FHWA

S.D.D. 8 A 5-19d

S.D.D. 8 A 5-19d

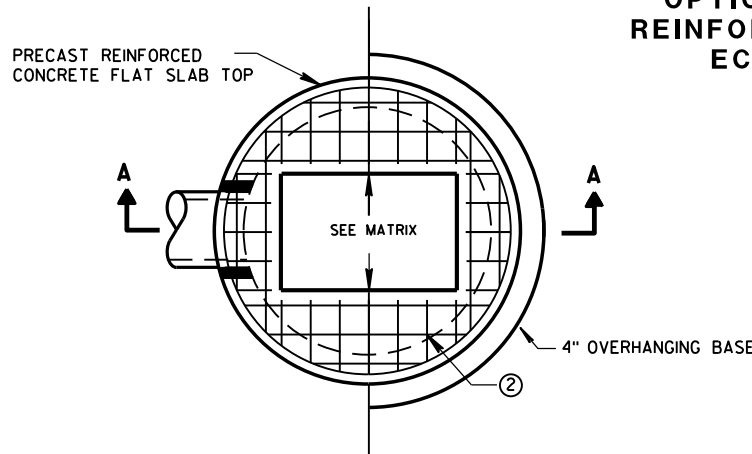


PLAN VIEW CIRCULAR OPENING

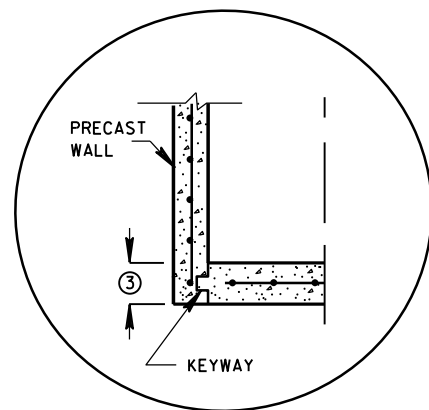


OPTIONAL PRECAST REINFORCED CONCRETE ECCENTRIC TOP

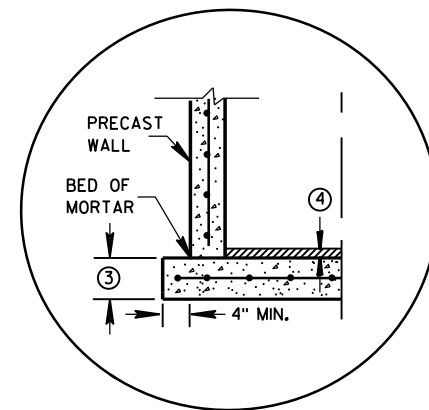
OPTIONAL PRECAST REINFORCED CONCRETE CONCENTRIC TOP



PLAN VIEW RECTANGULAR OPENING



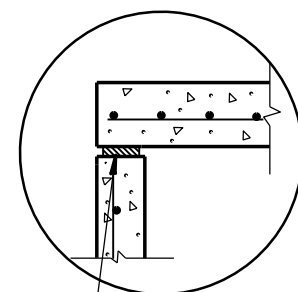
PRECAST REINFORCED CONCRETE WITH INTEGRAL BASE OPTION



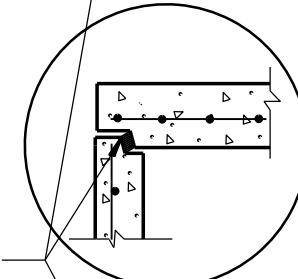
SEPARATE PRECAST REINFORCED CONCRETE BASE OPTION

DETAIL "A"

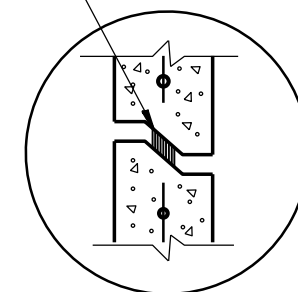
JOINTS TO BE SEALED WITH A BUTYL RUBBER SEAL PER SEALANT MANUFACTURERS RECOMMENDATIONS CONFORMING TO ASTM C 990 (TYP)



TOP WITH PLAIN END JOINT

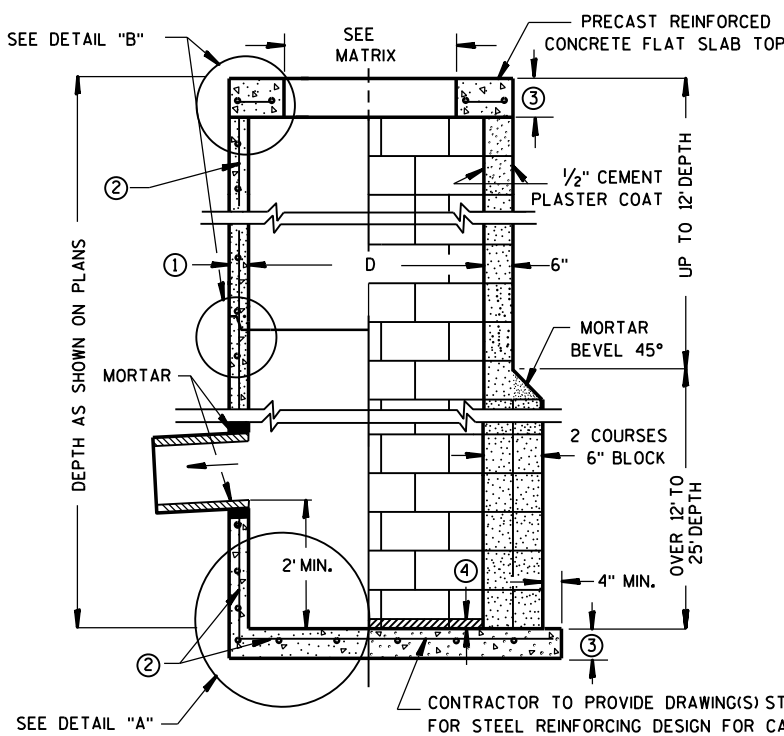


TOP WITH TONGUE AND GROOVE JOINT



RISER WITH TONGUE AND GROOVE JOINT

DETAIL "B"

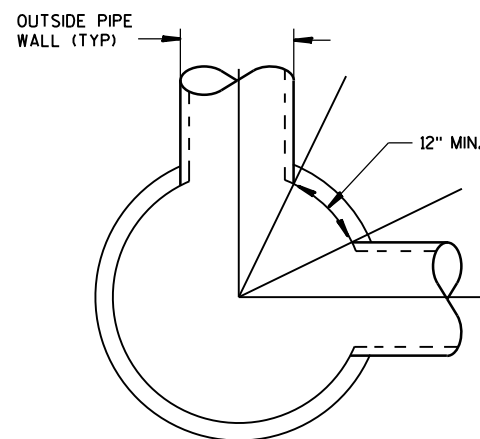


SECTION A-A

PRECAST REINFORCED CONCRETE WITH MONOLITHIC BASE

CONCRETE BLOCK WITH CAST-IN-PLACE OR PRECAST REINFORCED CONCRETE BASE ②

CONTRACTOR TO PROVIDE DRAWING(S) STAMPED BY A PROFESSIONAL ENGINEER FOR STEEL REINFORCING DESIGN FOR CAST-IN-PLACE STRUCTURES



DETAIL "C"

CATCH BASINS 3-FT, 4-FT, 5-FT AND 6-FT DIAMETER

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST CATCH BASIN UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL DRAINAGE STRUCTURES ARE DESIGNATED ON THE PLANS AS "MANHOLES 3X3-L", "CATCH BASINS 4-B", "INLETS 2X3-H", ETC. THE FIRST NUMBERS DESIGNATE THE SIZE OF THE STRUCTURE, AND THE FOLLOWING LETTER DESIGNATES THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF FOUNDATION BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

PRECAST REINFORCED CONCRETE CONE TOPS (ECCENTRIC OR CONCENTRIC) OR PRECAST REINFORCED CONCRETE FLAT SLAB TOPS MAY BE USED ON CONCRETE BLOCK STRUCTURES.

ECCENTRIC CONE TOPS MAY BE USED ON ALL STRUCTURES, AND CONCENTRIC CONE TOPS SHALL BE USED ONLY ON STRUCTURES 5 FEET OR LESS IN DEPTH, UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

STEPS MEETING AASHTO M199 AND THE FOLLOWING REQUIREMENTS SHALL BE INSTALLED IN ALL STRUCTURES OVER 5 FEET IN DEPTH; 16 INCH C-C MAXIMUM SPACING; PROJECT A MINIMUM CLEAR DISTANCE OF 4 INCHES FROM THE WALL AT THE POINT OF EMBEDMENT; MINIMUM LENGTH OF 10 INCHES; MINIMUM WALL EMBEDMENT OF 3 INCHES. FERROUS METAL STEPS NOT PAINTED OR TREATED TO RESIST CORROSION SHALL HAVE A MINIMUM CROSS SECTIONAL DIMENSION OF 1 INCH.

STEPS OF APPROVED POLYPROPYLENE PLASTIC COATED REINFORCEMENT BAR ARE ACCEPTABLE. REINFORCING BAR MUST BE A MINIMUM OF 1/2 INCH AND MEET THE REQUIREMENTS OF ASTM A615.

CERTIFICATION SHALL BE PROVIDED THAT INSTALLED STEPS WHEN TESTED IN ACCORDANCE WITH SECTION 10 OF AASHTO T280 CAN WITHSTAND A VERTICAL LOAD OF 800 LBS. AND A HORIZONTAL LOAD OF 400 LBS.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

ALL PRECAST INLET UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF AASHTO DESIGNATION M199.

PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

CONCRETE BLOCK WILL NOT BE PERMITTED FOR STRUCTURES GREATER THAN 4 FEET IN DIAMETER.

4" OVERHANGING BASES ARE REQUIRED FOR ALL CONCRETE BLOCK INSTALLATIONS. 4" OVERHANG IS REQUIRED WHEN SEPARATE PRECAST BASE IS PROVIDED. OVERHANG IS NOT REQUIRED ON PRECAST STRUCTURES WITH AN INTEGRAL OR MONOLITHIC BASE.

FOR ADDITIONAL CONFIGURATIONS, MAINTAIN A MINIMUM OF 12 INCHES AS MEASURED FROM THE INSIDE OF THE STRUCTURE WALL BETWEEN THE OUTSIDE PIPE WALLS OF ADJACENT PIPES. SEE DETAIL "C".

- ① MINIMUM WALL THICKNESS SHALL BE 4 INCHES FOR 3-FT, 5 INCHES FOR 4-FT, 6 INCHES FOR 5-FT AND 7 INCHES FOR 6-FT DIAMETER PRECAST CATCH BASINS.
- ② FOR PRECAST CATCH BASINS PROVIDE REINFORCING STEEL IN ACCORDANCE TO AASHTO M199.
- ③ PRECAST FLAT SLAB TOPS AND BASES WITH A DIAMETER OF 48" AND LESS SHALL HAVE A MINIMUM THICKNESS OF 6". PRECAST FLAT SLAB TOPS AND BASES WITH A DIAMETER LARGER THAN 48" SHALL HAVE A MINIMUM THICKNESS OF 8".
- ④ 1" CONCRETE KEY POURED AFTER INSTALLATION. 2" SUMP MEASURED FROM TOP OF KEY.

CATCH BASIN COVER OPENING MATRIX

CATCH BASIN SIZE	INLET COVER TYPE OPENING SIZE (FT)	ALL A'S	ALL B'S	BW	C	F	ALL H'S	S	T	V	WM	Z
3-FT	2X2	X	X					X		X		
	2 DIA.				X					X		X
4-FT- 6-FT	2X2	X	X					X		X		
	2X2.5			X				X	X	X	X	
	2 DIA.				X							X
	2X3						X					
	2.5X3											

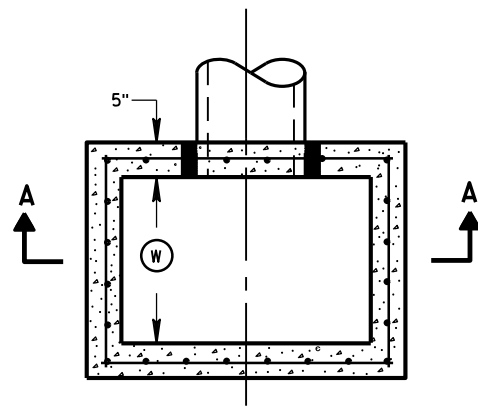
PIPE MATRIX

CATCH BASIN SIZE	MAXIMUM INSIDE PIPE DIAMETER FOR TWO PIPES	
	180° SEPARATION (IN)	90° SEPARATION (IN)
3-FT	15	12
4-FT	24	18
5-FT	36	24
6-FT	42	30

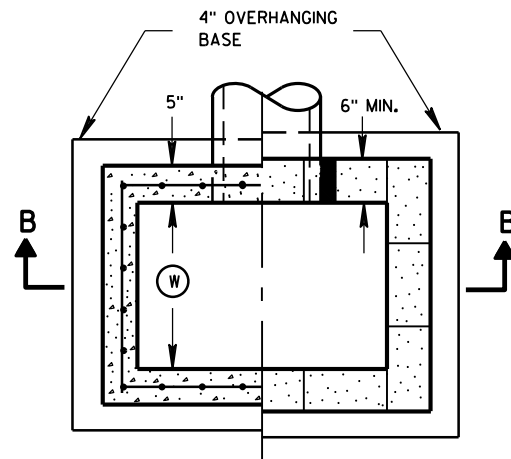
CATCH BASINS 3-FT, 4-FT, 5-FT AND 6-FT DIAMETER

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

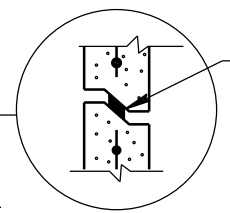
APPROVED
Sep 1, 2016 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
FHWA UNIT SUPERVISOR



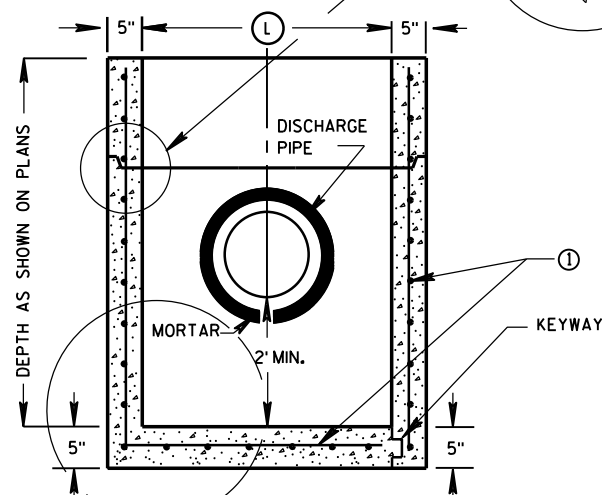
PLAN VIEW



PLAN VIEW

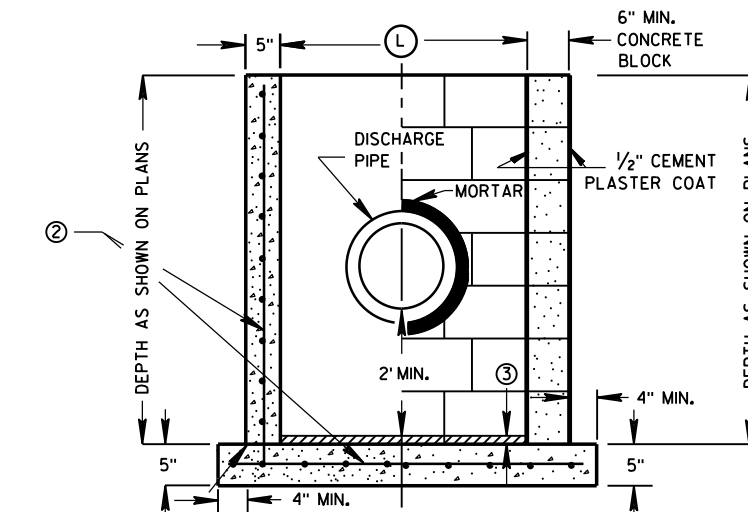


RISER JOINTS TO BE SEALED WITH A BUTYL RUBBER SEAL PER SEALANT MANUFACTURERS RECOMMENDATIONS CONFORMING TO ASTM C 990 (TYP)



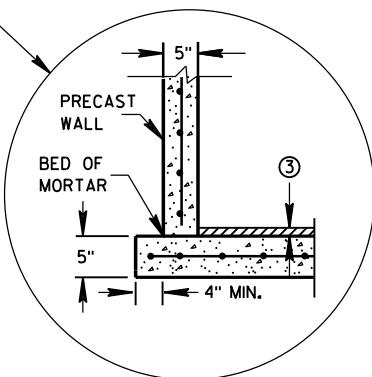
PRECAST REINFORCED CONCRETE WITH MONOLITHIC BASE

SECTION A-A



CONSTRUCTION JOINT
CAST-IN-PLACE REINFORCED CONCRETE
CONCRETE BLOCK ON CAST-IN-PLACE WITH PRECAST REINFORCED CONCRETE BASE ①

SECTION B-B



SEPARATE PRECAST REINFORCED CONCRETE BASE OPTION

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST CATCH BASIN UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL PRECAST CATCH BASIN UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF ASTM C 913.

ALL DRAINAGE STRUCTURES ARE DESIGNATED ON THE PLANS AS "MANHOLES 3X3-L", "CATCH BASINS 4-B", "INLETS 2X3-H", ETC. THE FIRST NUMBERS DESIGNATES THE SIZE OF THE STRUCTURE, AND THE FOLLOWING LETTER DESIGNATES THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF FOUNDATION BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

4" OVERHANGING BASES ARE REQUIRED FOR CAST-IN-PLACE REINFORCED CONCRETE AND CONCRETE BLOCK INSTALLATIONS. 4" OVERHANG IS REQUIRED WHEN SEPARATE PRECAST BASE IS PROVIDED.

OVERHANG IS NOT REQUIRED ON PRECAST STRUCTURES WITH AN INTEGRAL OR MONOLITHIC BASE.

MAXIMUM INSIDE PIPE DIAMETER DETERMINED BY 3" CLEARANCE ON EACH SIDE OF THE OUTSIDE WALL OF THE PIPE. SEE DETAIL "A". ASSUMES PIPE ENTERS PERPENDICULAR TO THE STRUCTURE.

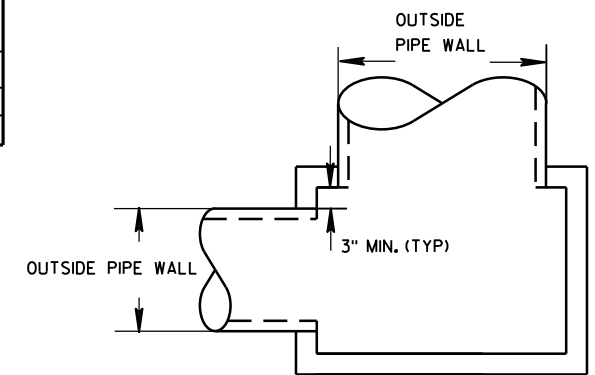
- ① FOR PRECAST CATCH BASINS PROVIDE REINFORCING STEEL IN ACCORDANCE TO ASTM C 913.
- ② CONTRACTOR TO PROVIDE DRAWING(S) STAMPED BY A PROFESSIONAL ENGINEER FOR STEEL REINFORCING DESIGN FOR CAST-IN-PLACE STRUCTURES.
- ③ 1" CONCRETE KEY POURED AFTER INSTALLATION. 2" SUMP MEASURED FROM TOP OF KEY.

CATCH BASIN COVER MATRIX

CATCH BASIN SIZE	INLET COVER TYPE		F	ALL H'S
	WIDTH (W) (FT)	LENGTH (L) (FT)		
2X3-FT	2	3		X
2.5X3-FT	2.5	3	X	

PIPE MATRIX

CATCH BASIN SIZE	MAXIMUM INSIDE PIPE DIAMETER FOR TWO PIPES	
	WIDTH (IN)	LENGTH (IN)
2X3-FT	12	24
2.5X3-FT	18	24



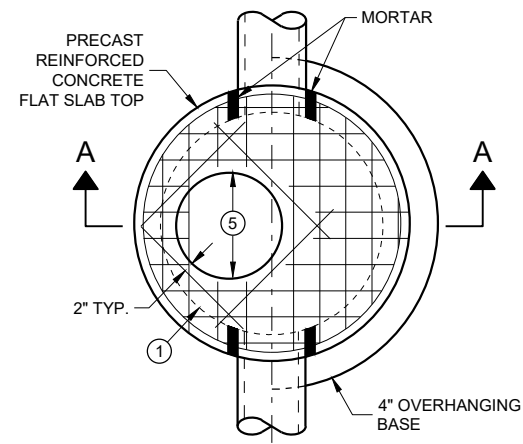
DETAIL "A"

CATCH BASINS 2X3-FT AND 2.5X3-FT

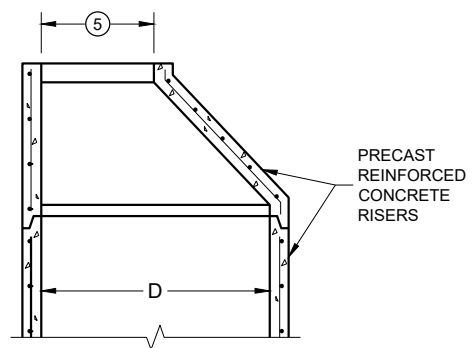
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE: Sept., 2016 /S/ Rodney Taylor
ROADWAY STANDARDS DEVELOPMENT
UNIT SUPERVISOR

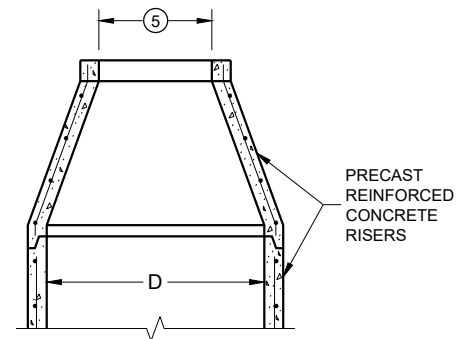
CATCH BASINS 2X3-FT AND 2.5X3-FT



**PLAN VIEW
CIRCULAR OPENING**



**OPTIONAL PRECAST
REINFORCED CONCRETE
ECCENTRIC TOP**



**OPTIONAL PRECAST
REINFORCED CONCRETE
CONCENTRIC TOP**

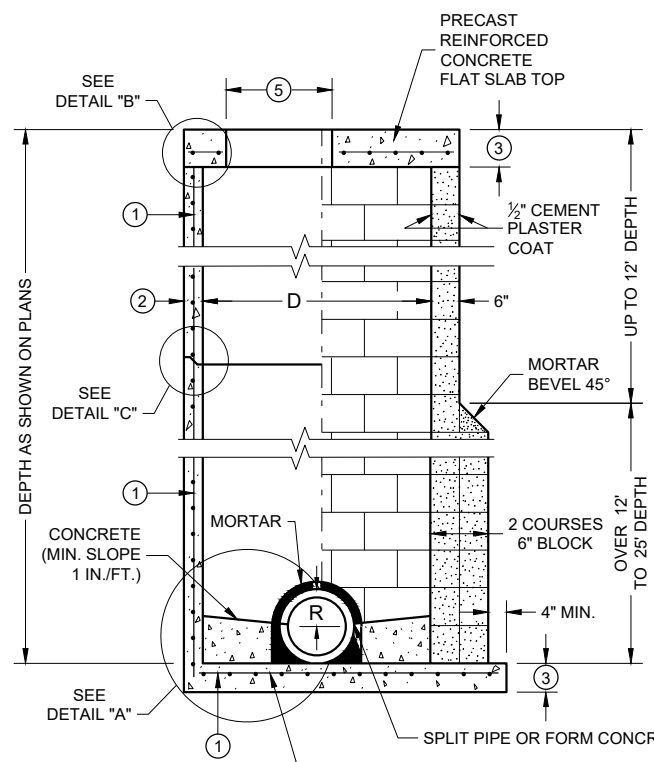
MANHOLE COVER OPENING MATRIX

MANHOLE COVER TYPE OPENING SIZE (FT.)	C	ALL J'S	K	L	M
2 DIA.	X	X		X	
3 DIA.			X		X

PIPE MATRIX

MANHOLE SIZE (DIA.)	MAXIMUM INSIDE PIPE DIAMETER FOR TWO PIPES		MINIMUM WALL THICKNESS (IN)	MINIMUM PRECAST FLAT SLAB TOP AND BASE THICKNESS
	180° SEPARATION (IN)	90° SEPARATION (IN)		
3-FT	15	12	4	6
4-FT	24	18	4	6
5-FT	36	24	5	8
6-FT	42	36	6	8
7-FT	48	36/42*	7	8
8-FT	60	42	8	8
9-FT	66	54	9	10
10-FT	72	60	10	10

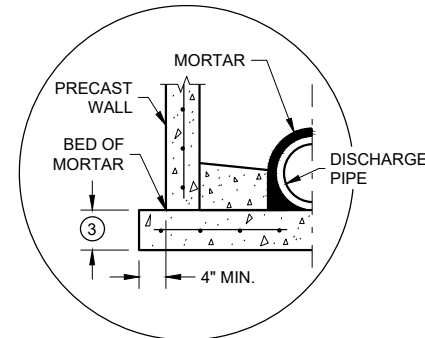
*A 36" PIPE AND A 42" PIPE CAN BE PLACED WITHIN 90 DEGREES. SEE MINIMUM HORIZONTAL PIPE SEPARATION DETAIL.



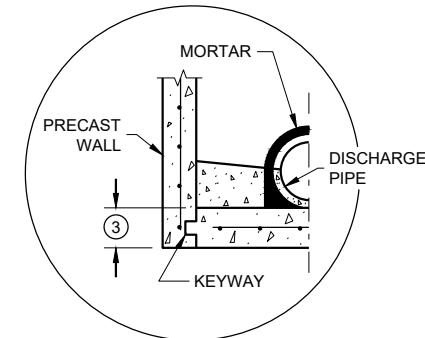
SECTION A - A

PRECAST REINFORCED CONCRETE WITH MONOLITHIC BASE

CONCRETE BLOCK WITH CAST IN PLACE OR PRECAST REINFORCED CONCRETE BASE ①

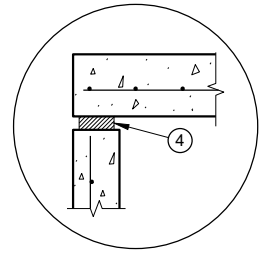


SEPARATE PRECAST REINFORCED CONCRETE BASE OPTION

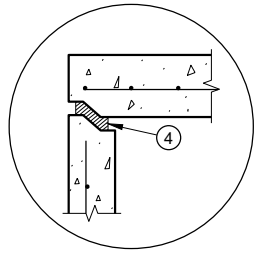


PRECAST REINFORCED CONCRETE WITH INTEGRAL BASE OPTION

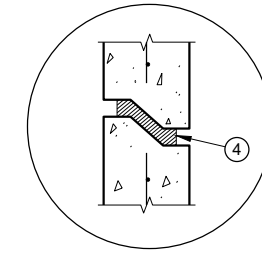
DETAIL "A"



TOP WITH PLAIN END JOINT



TOP WITH TONGUE AND GROOVE JOINT



RISER WITH TONGUE AND GROOVE JOINT

DETAIL "B"

DETAIL "C"

MANHOLES 3-FT, 4-FT, 5-FT, 6-FT, 7-FT, 8-FT, 9-FT AND 10-FT DIAMETER

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST MANHOLE UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL DRAINAGE STRUCTURES ARE DESIGNATED ON THE PLANS AS "MANHOLES 3X3-L", "CATCH BASINS 4-B", "INLETS 2X3-H", ETC. THE FIRST NUMBERS DESIGNATE THE SIZE OF THE STRUCTURE, AND THE FOLLOWING LETTER DESIGNATES THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF FOUNDATION BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

PRECAST REINFORCED CONCRETE CONE TOPS (ECCENTRIC OR CONCENTRIC) OR PRECAST REINFORCED CONCRETE FLAT SLAB TOPS MAY BE USED ON CONCRETE BLOCK STRUCTURES.

ECCENTRIC CONE TOPS MAY BE USED ON ALL STRUCTURES. CONCENTRIC CONE TOPS SHALL BE USED ONLY ON STRUCTURES 5 FEET OR LESS IN DEPTH UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

STEPS MEETING AASHTO M199 AND THE FOLLOWING REQUIREMENTS SHALL BE INSTALLED IN ALL STRUCTURES OVER 5 FEET IN DEPTH: 16 INCH C-C MAXIMUM SPACING; PROJECT A MINIMUM CLEAR DISTANCE OF 4 INCHES FROM THE WALL AT THE POINT OF EMBEDMENT; MINIMUM LENGTH OF 10 INCHES; MINIMUM WALL EMBEDMENT OF 3 INCHES. FERROUS METAL STEPS NOT PAINTED OR TREATED TO RESIST CORROSION SHALL HAVE A MINIMUM CROSS SECTIONAL DIMENSION OF 1 INCH.

STEPS OF APPROVED POLYPROPYLENE PLASTIC COATED REINFORCEMENT BAR ARE ACCEPTABLE. REINFORCING BAR MUST BE A MINIMUM OF 1/2 INCH AND MEET THE REQUIREMENTS OF ASTM A615.

CERTIFICATION SHALL BE PROVIDED THAT INSTALLED STEPS WHEN TESTED IN ACCORDANCE WITH SECTION 10 OF AASHTO T280 CAN WITHSTAND A VERTICAL LOAD OF 800 LBS. AND A HORIZONTAL LOAD OF 400 LBS.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

ALL PRECAST MANHOLE UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF AASHTO DESIGNATION M199.

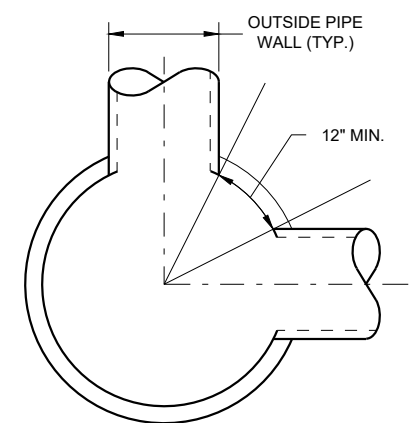
PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

CONCRETE BLOCK WILL NOT BE PERMITTED FOR STRUCTURES GREATER THAN 4 FEET IN DIAMETER.

4" OVERHANGING BASES ARE REQUIRED FOR ALL CONCRETE BLOCK INSTALLATIONS. 4" OVERHANG IS REQUIRED WHEN SEPARATE PRECAST BASE IS PROVIDED. OVERHANG IS NOT REQUIRED ON PRECAST STRUCTURES WITH AN INTEGRAL OR MONOLITHIC BASE.

FOR ADDITIONAL CONFIGURATIONS, MAINTAIN A MINIMUM OF 12 INCHES AS MEASURED FROM THE INSIDE OF THE STRUCTURE WALL BETWEEN THE OUTSIDE PIPE WALLS OF ADJACENT PIPES. SEE DETAIL "D".

- ① FOR PRECAST MANHOLES PROVIDE REINFORCING STEEL IN ACCORDANCE TO AASHTO M199.
- ② SEE PIPE MATRIX TABLE FOR MINIMUM WALL THICKNESS FOR PRECAST MANHOLES
- ③ SEE PIPE MATRIX TABLE FOR MINIMUM THICKNESS OF PRECAST FLAT SLAB TOPS AND BASES.
- ④ JOINTS TO BE SEALED WITH A BUTYL RUBBER SEAL PER SEALANT MANUFACTURERS RECOMMENDATIONS CONFORMING TO ASTM C 990 (TYP.).
- ⑤ SEE MANHOLE COVER OPENING MATRIX.

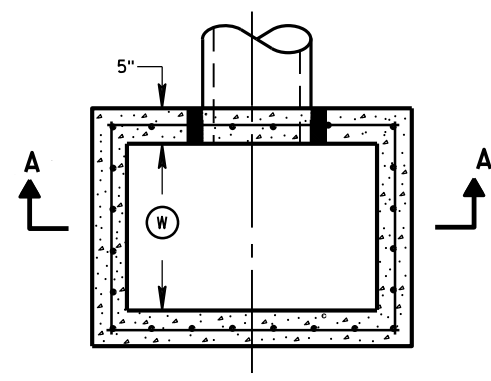


MINIMUM HORIZONTAL PIPE SEPARATION

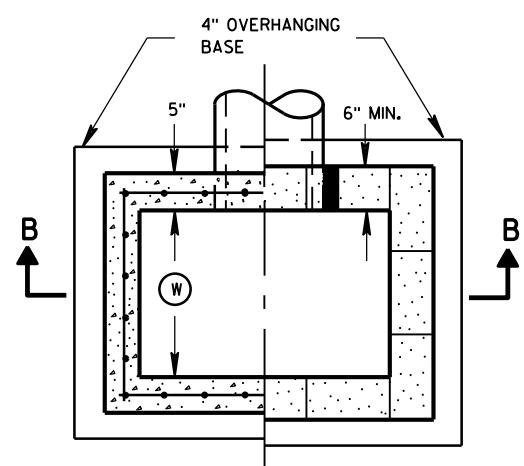
**MANHOLES, 3-FT, 4-FT
5-FT, 6-FT, 7-FT, 8-FT, 9-FT
AND 10-FT DIAMETER**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

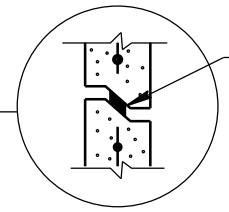
APPROVED
November 2021 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
ENGINEER
FHWA



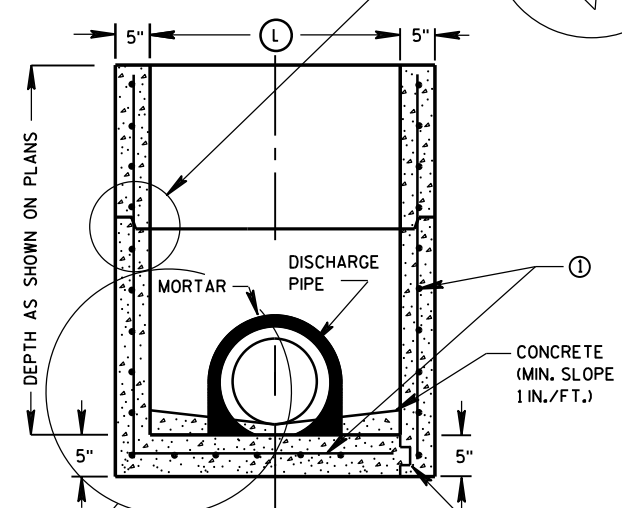
PLAN VIEW



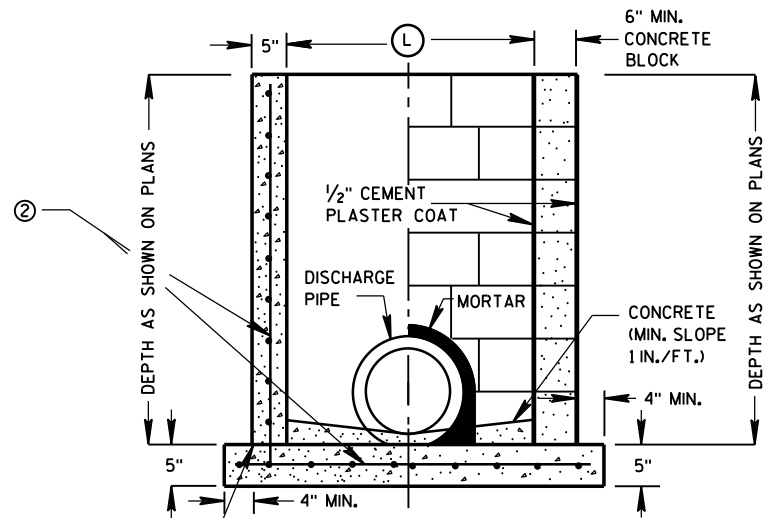
PLAN VIEW



RISER JOINTS TO BE SEALED WITH A BUTYL RUBBER SEAL PER SEALANT MANUFACTURERS RECOMMENDATIONS CONFORMING TO ASTM C 990 (TYP)



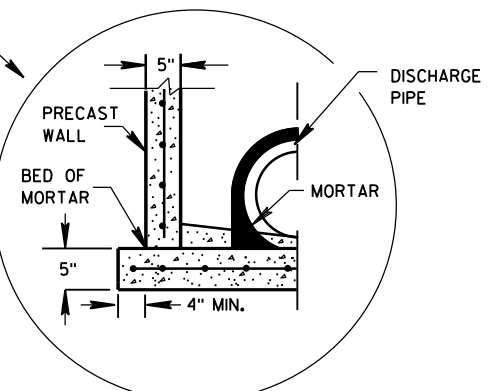
SECTION A-A



SECTION B-B

PRECAST REINFORCED CONCRETE WITH MONOLITHIC BASE
 PRECAST REINFORCED CONCRETE WITH INTEGRAL BASE
 KEYWAY

CAST-IN-PLACE REINFORCED CONCRETE
 CONCRETE BLOCK WITH CAST-IN-PLACE OR PRECAST REINFORCED CONCRETE BASE ①



SEPARATE PRECAST REINFORCED CONCRETE BASE OPTION

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

UNLESS OTHERWISE AUTHORIZED IN WRITING BY THE ENGINEER, THE CONTRACTOR SHALL NOT ORDER AND DELIVER PRECAST INLET UNITS REQUIRED FOR THE PROJECT UNTIL A LIST OF SIZES IS FURNISHED BY THE ENGINEER.

DETAILED DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR UNDERGROUND DRAINAGE STRUCTURES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ALL PRECAST INLET UNITS SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF ASTM C 913.

ALL DRAINAGE STRUCTURES ARE DESIGNATED ON THE PLANS AS "MANHOLES 3X3-L", "CATCH BASINS 4-B", "INLETS 2X3-H", ETC. THE FIRST NUMBERS DESIGNATES THE SIZE OF THE STRUCTURE, AND THE FOLLOWING LETTER DESIGNATES THE TYPE OF COVER TO BE USED TO COMPRISE THE COMPLETE UNIT.

BASES SHALL BE PLACED ON A BED OF MATERIAL AT LEAST 6 INCHES IN DEPTH, WHICH MEETS THE REQUIREMENTS OF FOUNDATION BACKFILL. THIS BEDDING SHALL BE COMPACTED AND PROVIDE UNIFORM SUPPORT FOR THE ENTIRE AREA OF THE BASE.

ALL BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2 INCHES CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

PRECAST REINFORCED RISERS SHALL HAVE A TONGUE AND GROOVE JOINT WITH TONGUE UP OR DOWN.

4" OVERHANGING BASES ARE REQUIRED FOR CAST-IN-PLACE REINFORCED CONCRETE AND CONCRETE BLOCK INSTALLATIONS. 4" OVERHANG IS REQUIRED WHEN SEPARATE PRECAST BASE IS PROVIDED. OVERHANG IS NOT REQUIRED ON PRECAST STRUCTURES WITH AN INTEGRAL OR MONOLITHIC BASE.

MAXIMUM INSIDE PIPE DIAMETER DETERMINED BY 3 INCH CLEARANCE ON EACH SIDE OF THE OUTSIDE WALL OF THE PIPE. SEE DETAIL "A". ASSUMES PIPE ENTERS PERPENDICULAR TO THE STRUCTURE.

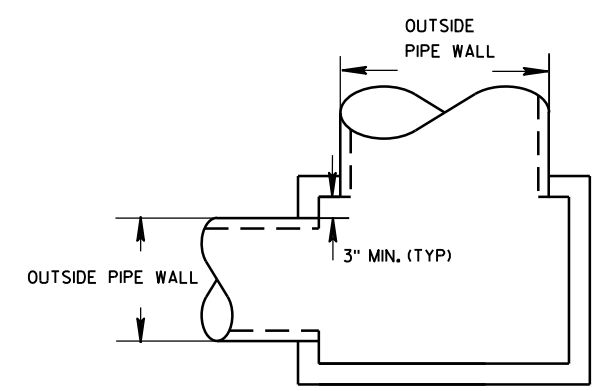
- ① FOR PRECAST INLETS PROVIDE REINFORCING STEEL IN ACCORDANCE TO ASTM C 913.
- ② CONTRACTOR TO PROVIDE DRAWING(S) STAMPED BY A PROFESSIONAL ENGINEER FOR STEEL REINFORCING DESIGN FOR CAST-IN-PLACE STRUCTURES.

INLET COVER MATRIX

INLET SIZE	INLET COVER TYPE		ALL A'S	ALL B'S	BW	F	ALL H'S	S	T	V	WM
	WIDTH ① (FT)	LENGTH ② (FT)									
2X2-FT	2	2	X	X				X		X	
2X2.5-FT	2	2.5			X			X	X	X	X
2X3-FT	2	3					X				
2.5X3-FT	2.5	3				X					

PIPE MATRIX

INLET SIZE	MAXIMUM INSIDE PIPE DIAMETER	
	WIDTH (IN)	LENGTH (IN)
2X2-FT	12	12
2X2.5-FT	12	18
2X3-FT	12	24
2.5X3-FT	18	24



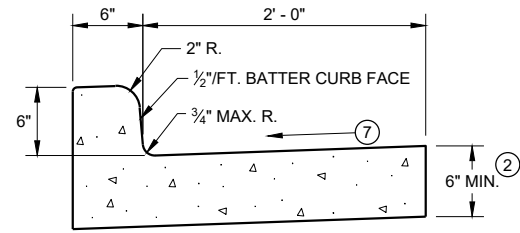
DETAIL "A"

INLETS 2X2-FT, 2X2.5-FT, 2X3-FT AND 2.5X3-FT

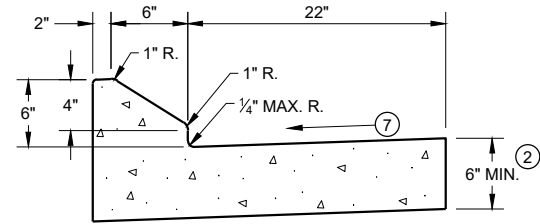
**INLETS 2X2-FT, 2X2.5-FT,
2X3-FT AND 2.5X3-FT**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

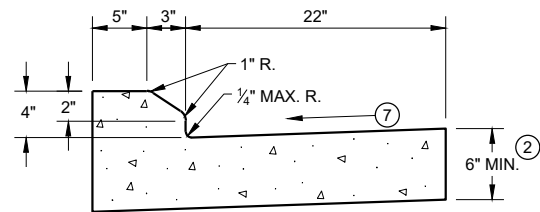
APPROVED
 Sep 1, 2016 /S/ Rodney Taylor
 DATE ROADWAY STANDARDS DEVELOPMENT
 FHWA UNIT SUPERVISOR



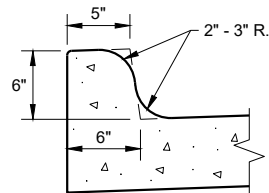
TYPES A^① & D



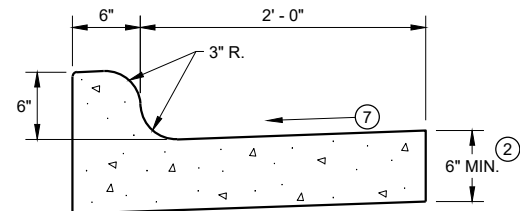
6" SLOPED CURB TYPES G^① & J



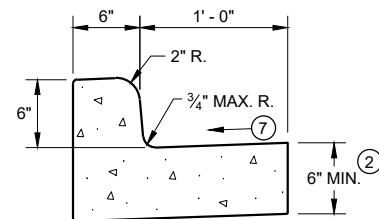
4" SLOPED CURB TYPES G^① & J



TYPES K^① & L
(OPTIONAL CURB SHAPE)

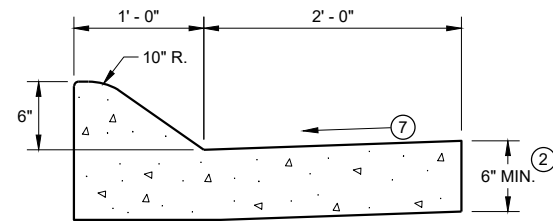


TYPES K^① & L
CONCRETE CURB AND GUTTER 30"

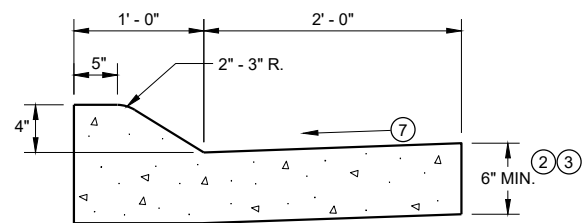


TYPES A^① & D

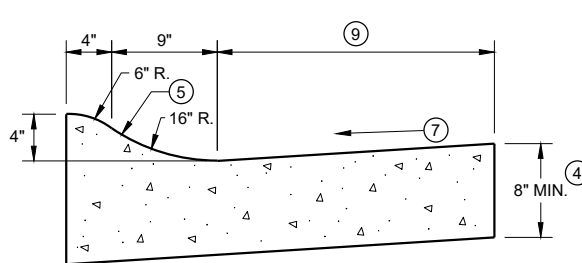
CONCRETE CURB AND GUTTER 18"



6" SLOPED CURB TYPES A^① & D

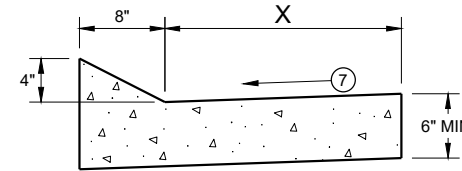


4" SLOPED CURB TYPES A^① & D
CONCRETE CURB AND GUTTER 36"



4" SLOPED CURB TYPES R^① & T

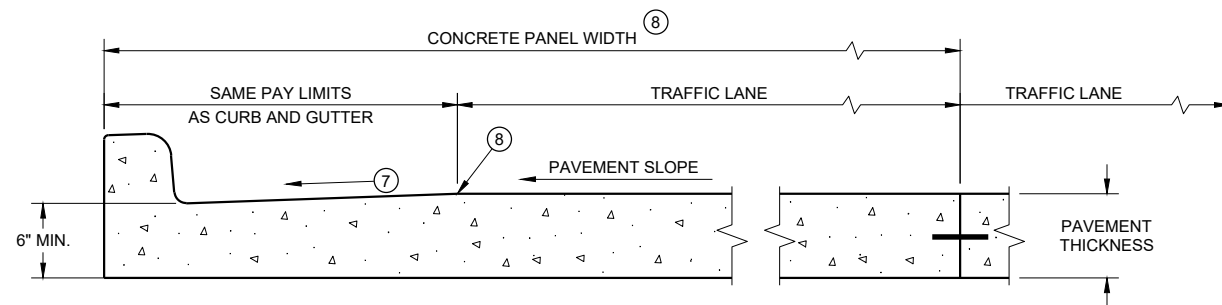
TBT & TBTT	X
30"	22"
36"	28"



TYPES TBT & TBTT^①
CONCRETE CURB AND GUTTER

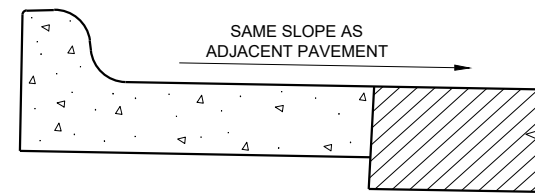
PAVEMENT THICKNESS AND MAXIMUM CONCRETE PANEL WIDTH TABLE

PAVEMENT THICKNESS	MAXIMUM PANEL WIDTH
LESS THAN 10"	12'
10" & ABOVE	15'



PARTIAL SECTION OF PAVEMENT* WITH INTEGRAL CURB AND GUTTER

* BIKE LANE IS NOT SHOWN



REVERSE SLOPE GUTTER^⑥
(TYPICAL FOR ALL CURB & GUTTER TYPES)

GENERAL NOTES

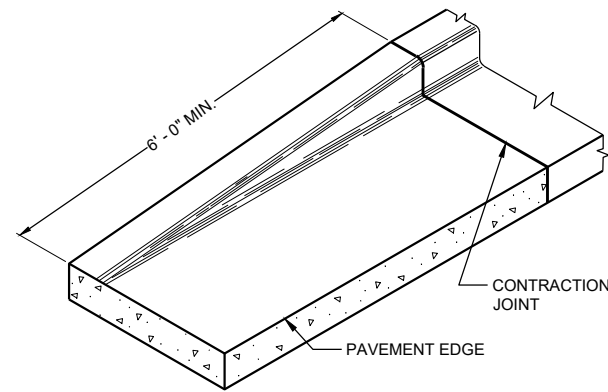
DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

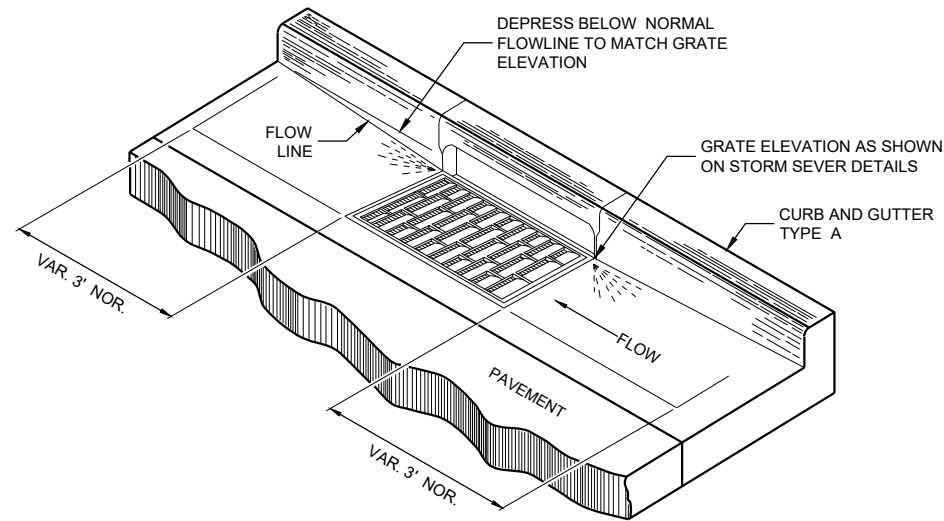
INTEGRAL CURB AND GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB AND GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ③ USE 8" MINIMUM GUTTER THICKNESS WHEN USED WITH AN ADJACENT CONCRETE TRUCK APRON PLACED BEHIND BACK OF CURB.
- ④ THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑤ UNLESS OTHERWISE NOTED, FOR STAKING PURPOSES THE FACE OF CURB IS 6" FROM THE BACK OF CURB.
- ⑥ WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.
- ⑦ USE 4% GUTTER CROSS SLOPE UNLESS OTHERWISE NOTED IN THE PLANS.
- ⑧ INCLUDE LONGITUDINAL JOINT AND TIE BARS ALONG LANE EDGE WHEN CONCRETE PANEL WIDTH EXCEEDS THE MAXIMUM WIDTH PER TABLE BELOW. LONGITUDINAL JOINT(S) ARE NOT ALLOWED WITHIN TRAFFIC LANES AND BIKE LANES. LONGITUDINAL JOINT MAY BE SAWED.
- ⑨ CONCRETE CURB AND GUTTER 4-INCH SLOPED 30-INCH TYPE "R" AND "T" = 17 INCHES
CONCRETE CURB AND GUTTER 4-INCH SLOPED 36-INCH TYPE "R" AND "T" = 23 INCHES



END SECTION CURB AND GUTTER



DETAIL OF CURB AND GUTTER AT INLETS

(TYPICAL H INLET COVER SHOWN)

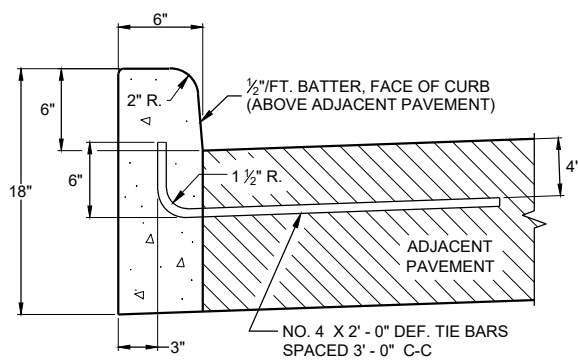
GENERAL NOTES

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

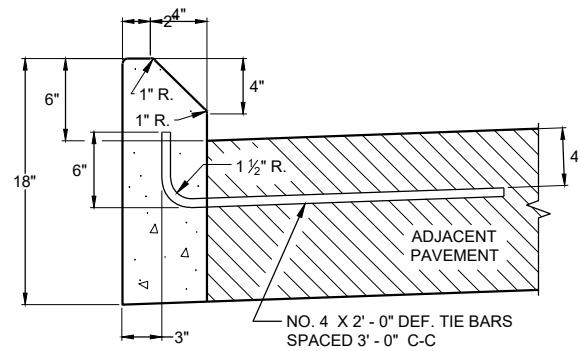
PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

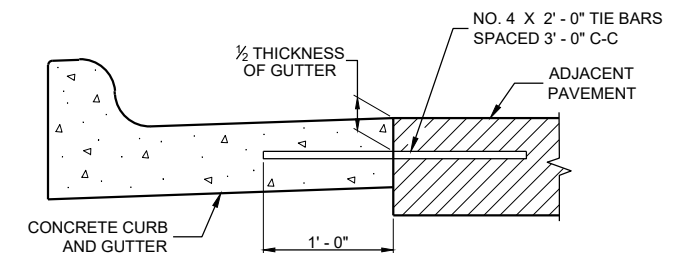
- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑩ REFER TO SDD 08D18 AND 08D19 FOR ADDITIONAL DRIVEWAY ENTRANCE CURB DETAILS.
- ⑪ PLACE 1" THICK EXPANSION JOINT MATERIAL BETWEEN VERTICAL FACE CURB TYPES EXTENDING FROM THE TOP OF CURB TO 1 INCH BELOW THE ADJOINING CONCRETE SURFACE. RIGID CONCRETE STRUCTURES INCLUDE RAISED CONCRETE MEDIANS, CONCRETE SAFETY ISLANDS, SPLITTER ISLANDS, OR LOCATIONS IDENTIFIED ON THE PLANS.



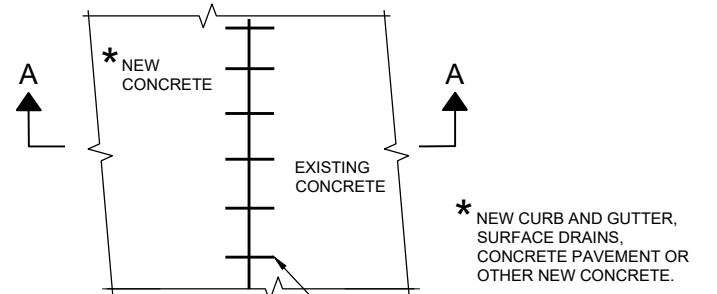
TYPES A^① & D



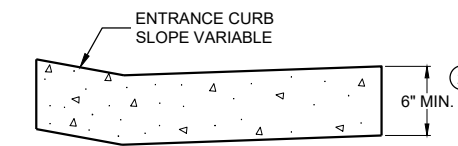
**TYPES G^① & J
CONCRETE CURB**



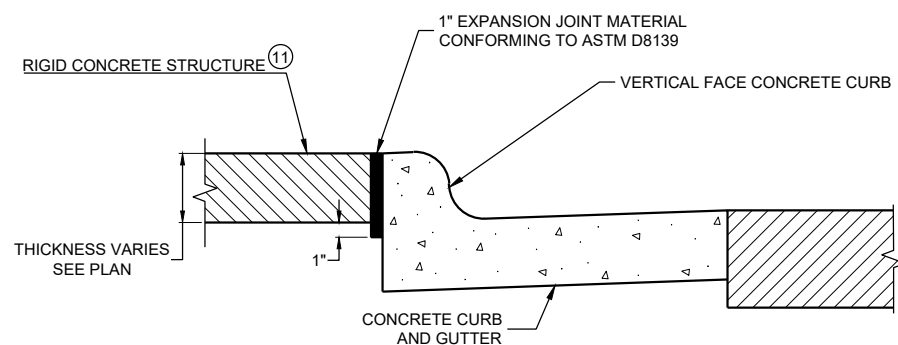
TYPICAL TIE BAR LOCATION^①



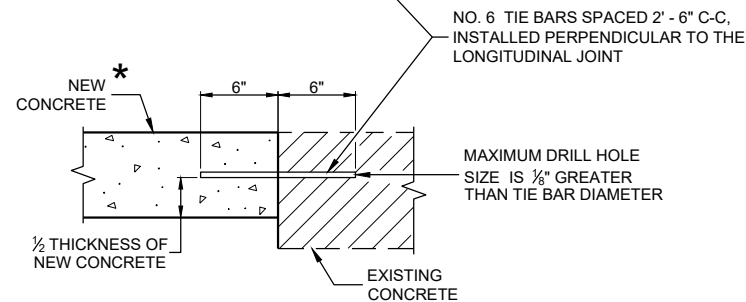
PLAN VIEW



**DRIVEWAY ENTRANCE CURB^⑩
(WHEN DIRECTED BY THE ENGINEER)**



EXPANSION JOINT DETAIL FOR VERTICAL CURB ABUTTING A RIGID STRUCTURE^⑪



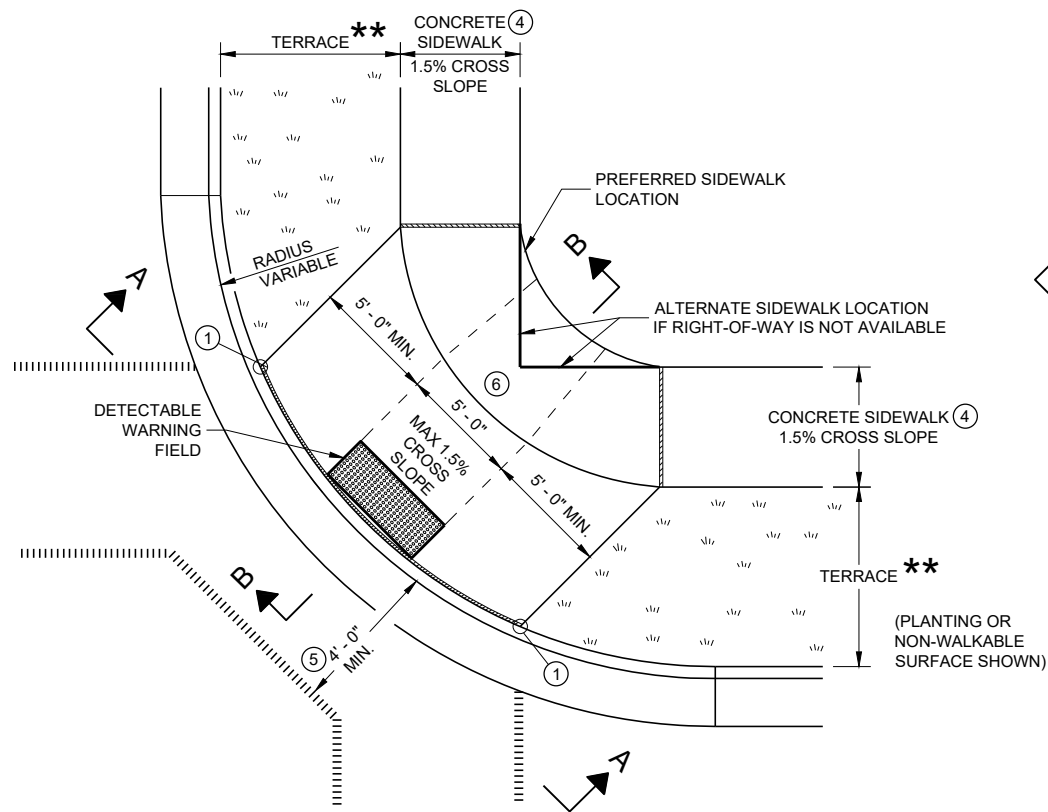
**SECTION A - A
TIE BARS DRILLED INTO EXISTING PAVEMENT**

CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS

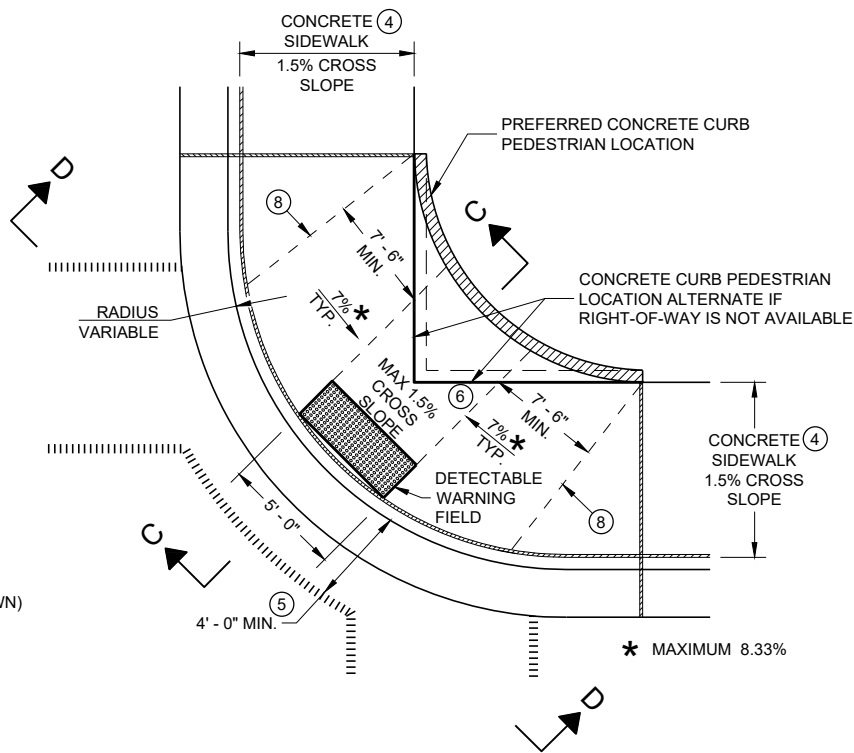
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE May 2023 /S/ Rodney Taylor
ROADWAY STANDARDS DEVELOPMENT ENGINEER

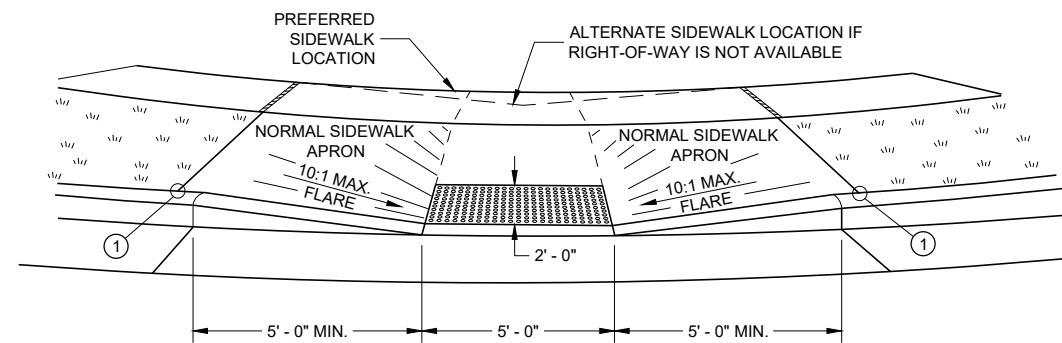
FHWA



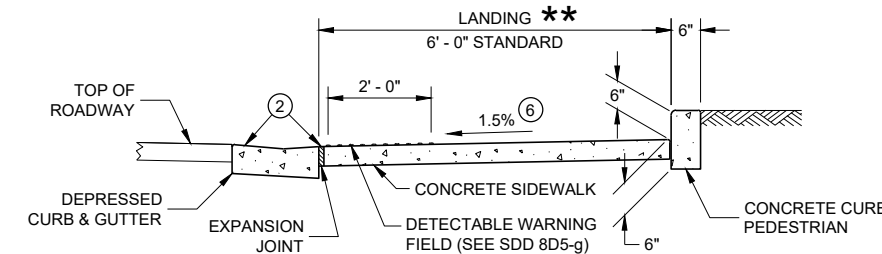
**PLAN VIEW
CURB RAMP TYPE 1
(CENTER OF CORNER RADIUS)**



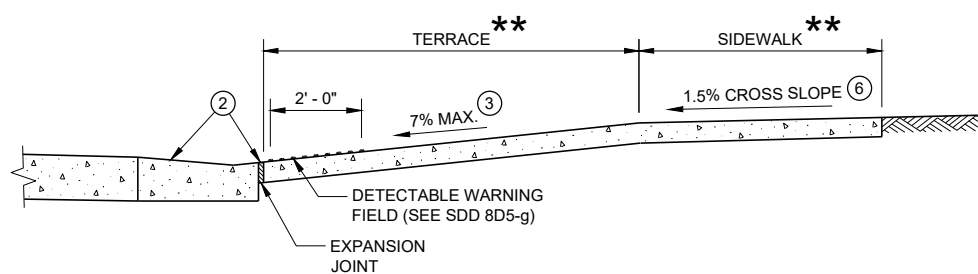
**PLAN VIEW
CURB RAMP TYPE 1 - A
(NO TERRACE)**



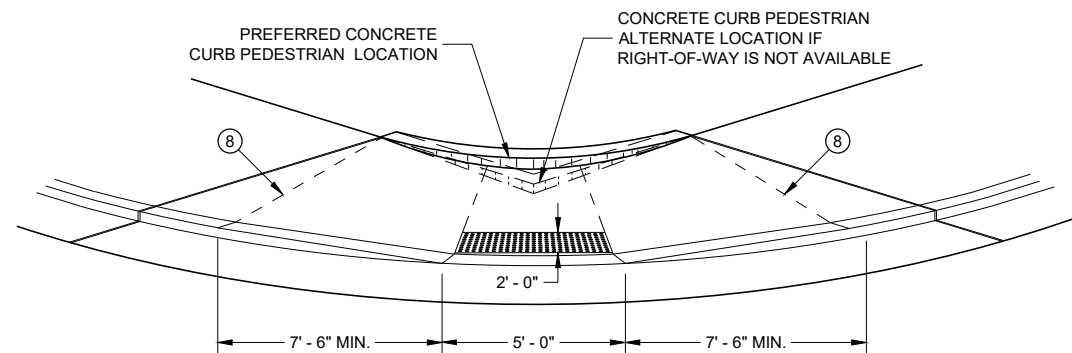
VIEW A - A FOR TYPE 1



SECTION C - C FOR TYPE 1 - A



SECTION B - B FOR TYPE 1



VIEW D - D FOR TYPE 1 - A

GENERAL NOTES

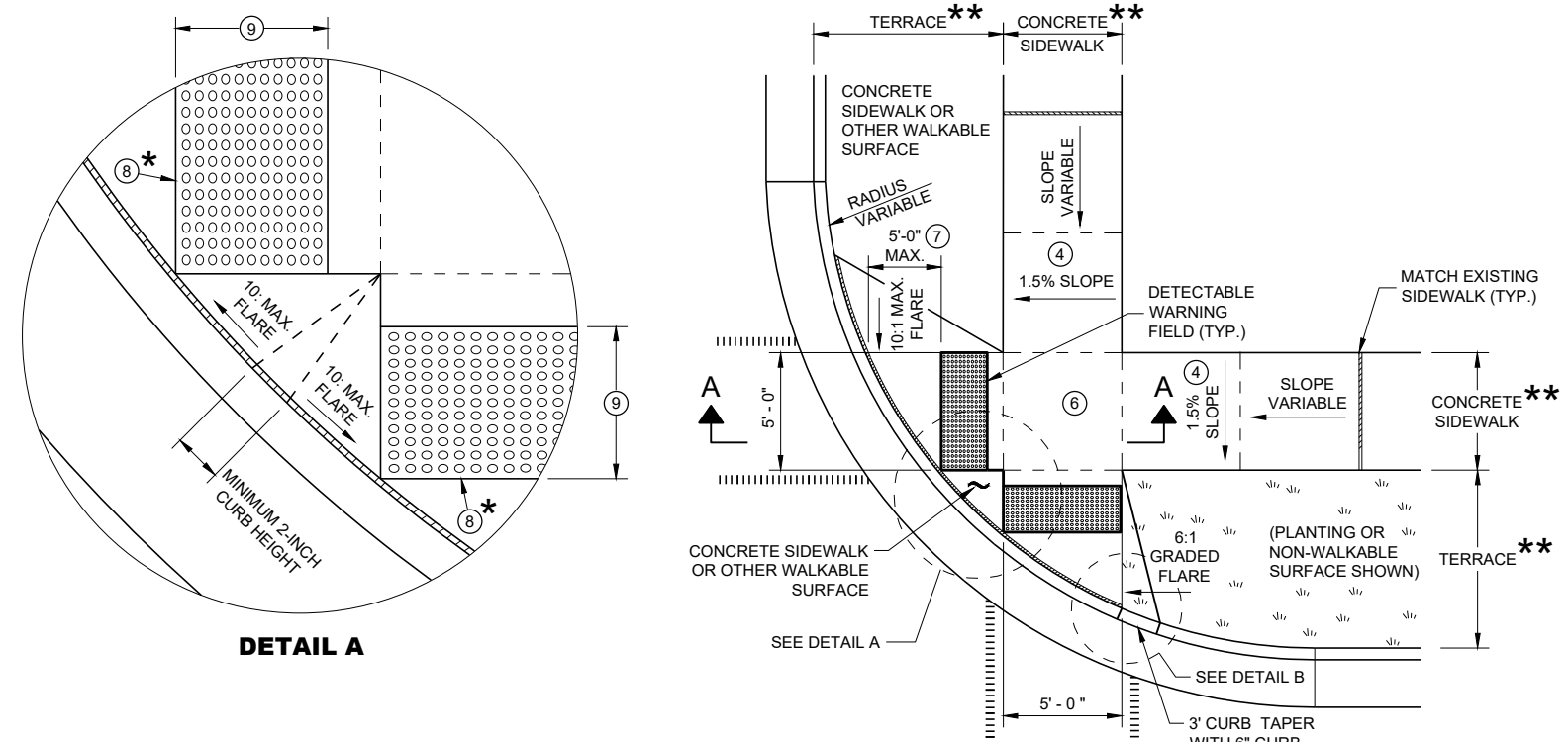
- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- WHEN NECESSARY, THE SIDEWALK ELEVATION MAY BE LOWERED TO MEET THE HIGH POINT ON THE RAMP.
- TYPE 1 CURB RAMPS SHALL HAVE A NORMAL SIDEWALK APRON AND CURB ON BOTH SIDES OF RAMP.
- DETECTABLE WARNING FIELD SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS "CURB RAMP DETECTABLE WARNING FIELD". THE CONCRETE PEDESTRIAN CURB, IF NEEDED, SHALL BE MEASURED AND PAID BY THE LINEAR FOOT AS "CONCRETE CURB PEDESTRIAN". CONCRETE SIDEWALK IN THE CURB RAMP AREA SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS CONCRETE SIDEWALK, INCLUDING THE AREA UNDER THE DETECTABLE WARNING FIELD.
- SELECT CURB RAMP DETECTABLE WARNING FIELD MATERIALS AND DEVICES FROM THE DEPARTMENT'S APPROVED MATERIALS LIST. THE COLOR OF THE DETECTABLE WARNING FIELD IS SPECIFIED ELSEWHERE AND IS INCIDENTAL TO THE BID ITEM OF "CURB RAMP DETECTABLE WARNING FIELD"
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- SURFACE TEXTURE OF THE RAMP SHALL BE OBTAINED BY COARSE BROOMING TRANSVERSE TO THE SLOPE OF THE RAMP.
- ① THIS POINT IS AN EXTENSION OF OUTSIDE EDGE OF APPROACHING SIDEWALK WHERE IT MEETS THE BACK OF CONCRETE CURB. POINT LOCATION MAY BE ADJUSTED TO ALIGN WITH BEGINNING OF FULL-HEIGHT CURB IF THIS DISTANCE IS SHORT.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4" INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ MAXIMUM 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑤ PROVIDE A LEVEL LANDING IN THE STREET AND GUTTER AREA (2% MAXIMUM SLOPE IN ANY DIRECTION). WHEN THE GUTTER SLOPE EXCEEDS 2%, CONSTRUCT THE LEVEL LANDING IN THE STREET AREA.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

LEGEND

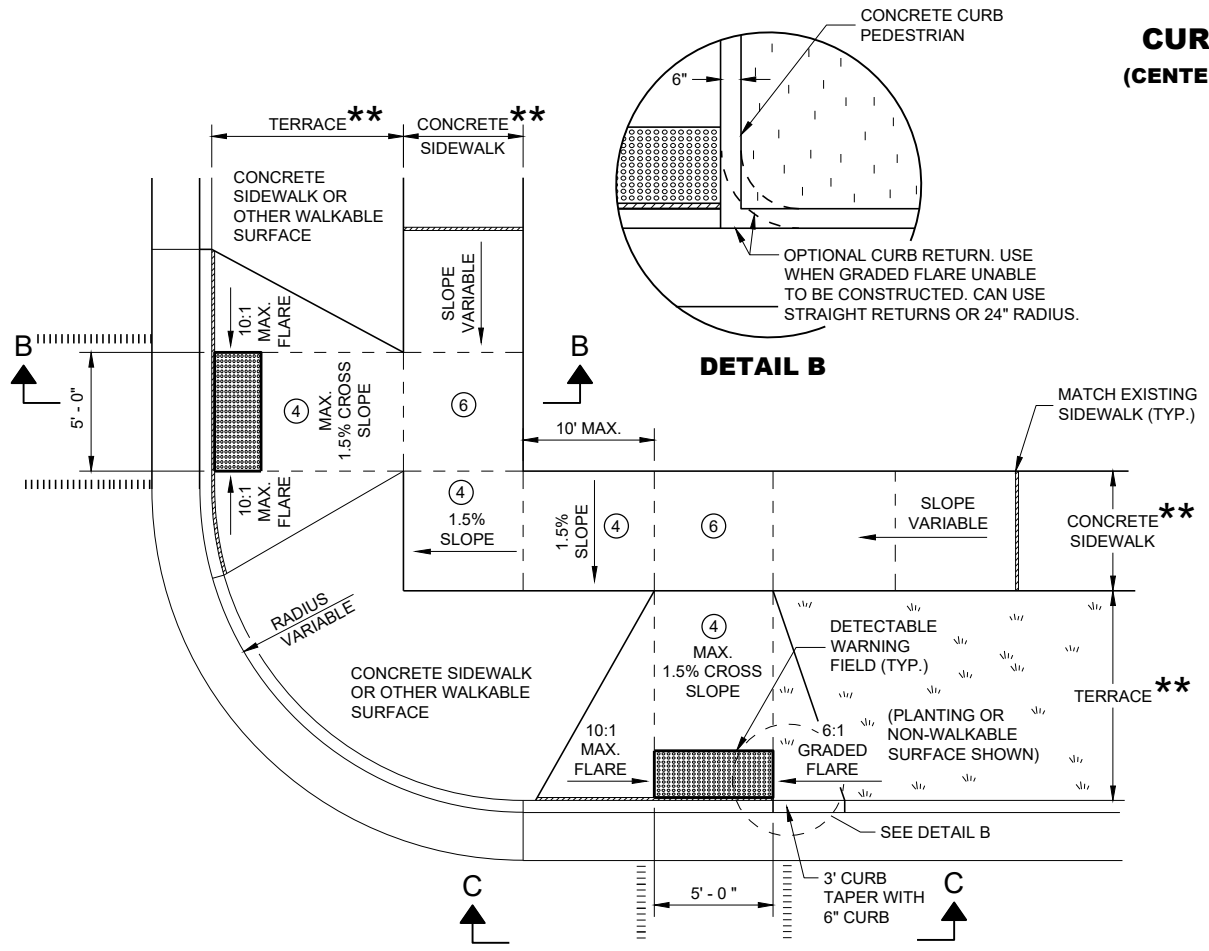
- 1/2" EXPANSION JOINT SIDEWALK
- - - - - CONTRACTION JOINT FIELD LOCATED
- ||||||| PAVEMENT MARKING CROSSWALK (WHITE)

**CURB RAMPS
TYPE 1 AND 1-A**

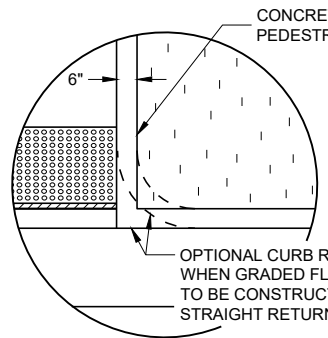
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION



PLAN VIEW CURB RAMP TYPE 2 (CENTER OF CORNER RADIUS)



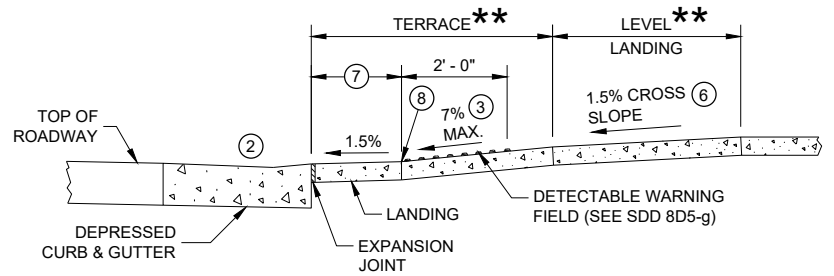
PLAN VIEW CURB RAMP TYPE 3 (OUTSIDE OF CROSSWALK AREA)



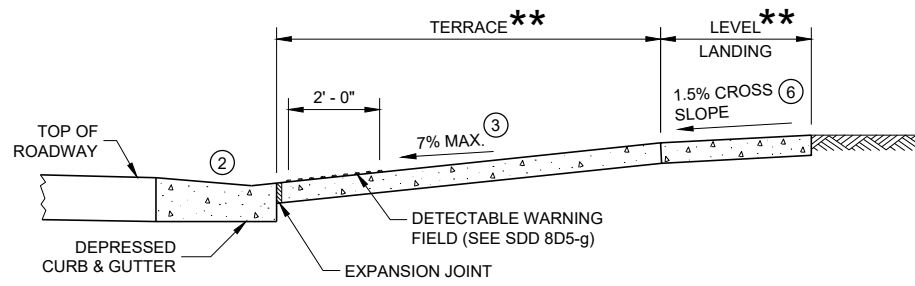
DETAIL B

GENERAL NOTES

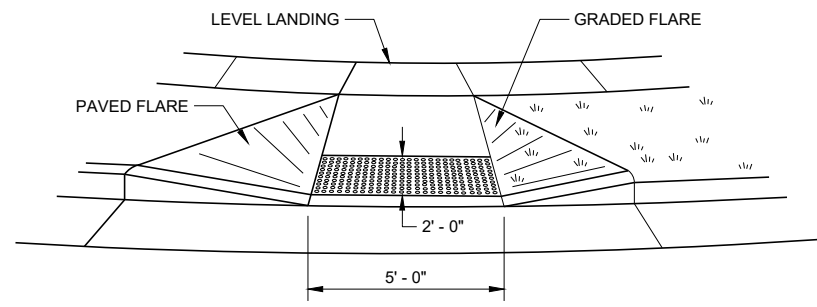
- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE (2.67% OR LESS) AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET X 5 FEET.
- ⑦ WHEN GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑨ WHEN DISTANCE IS LESS THAN 6' - 0", IT MAY BE DIFFICULT TO ACHIEVE A 7% SLOPE OR FLATTER ALONG THE RAMP. REDUCE CURB HEIGHT IN TRIANGLE AREA TO ACHIEVE 7% SLOPE OR FLATTER ON RAMP. CONSTRUCT 2-INCH MINIMUM CURB HEIGHT BETWEEN 10:1 FLARES.



SECTION A - A FOR TYPE 2



SECTION B - B FOR TYPE 3



VIEW C - C FOR TYPE 3

* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK

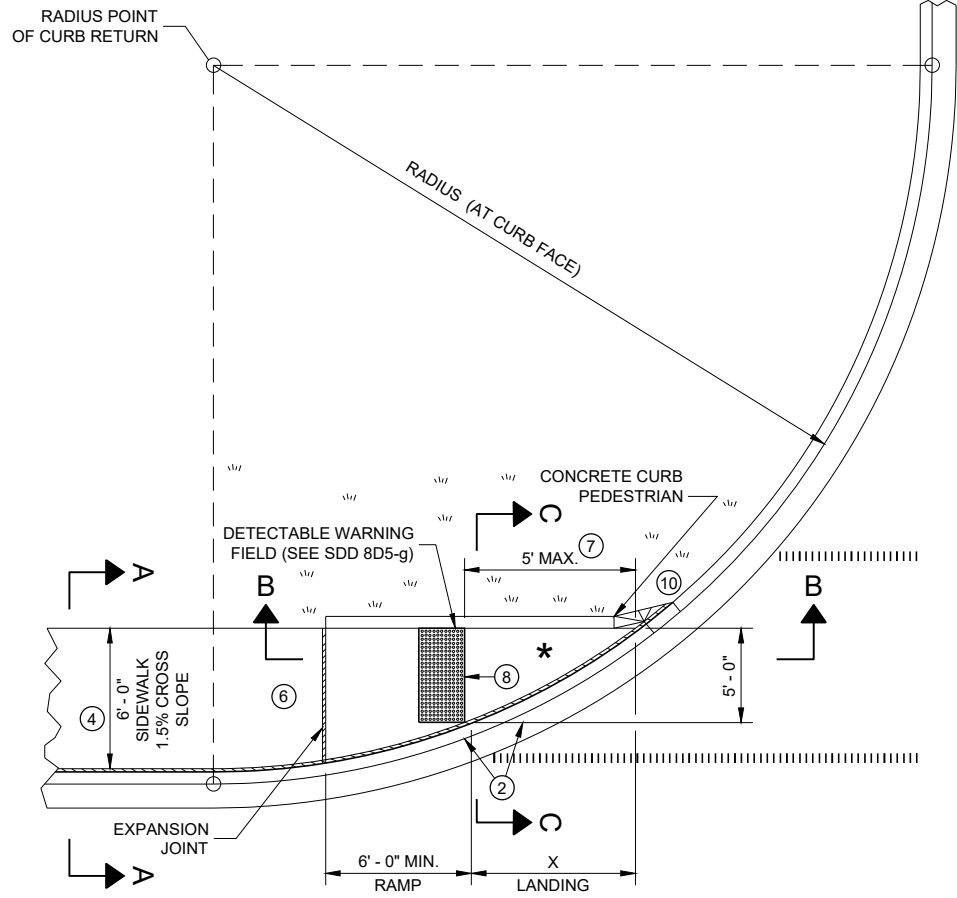
** WIDTH SHOWN ELSEWHERE IN THE PLANS

LEGEND

- 1/2" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT SIDEWALK
- PAVEMENT MARKING CROSSWALK (WHITE)

CURB RAMPS TYPE 2 AND 3

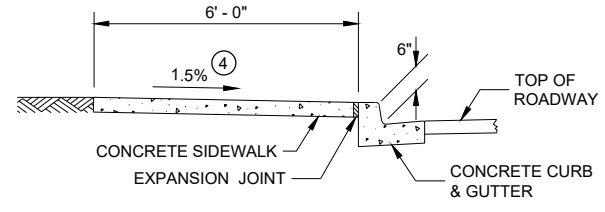
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**PLAN VIEW
CURB RAMP TYPE 4A**

RADIUS (AT CURB FACE)	X
10 FEET	4' - 7"
15 FEET	6' - 5 1/2"

INTERMEDIATE RADII CAN BE INTERPOLATED



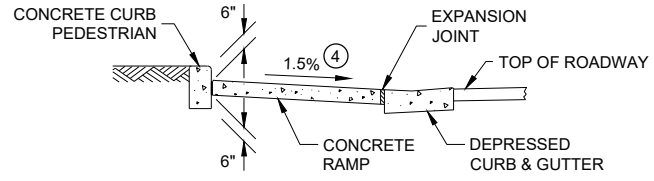
SECTION A - A FOR TYPE 4A

GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4" INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑦ WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑩ INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

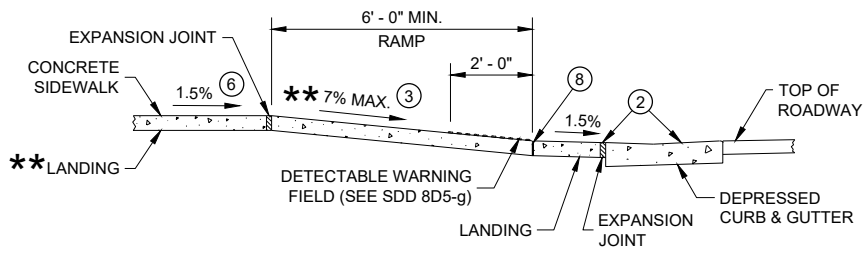
LEGEND

- 1/2" EXPANSION JOINT SIDEWALK
- - - CONTRACTION JOINT SIDEWALK
- ||||| PAVEMENT MARKING CROSSWALK (WHITE)



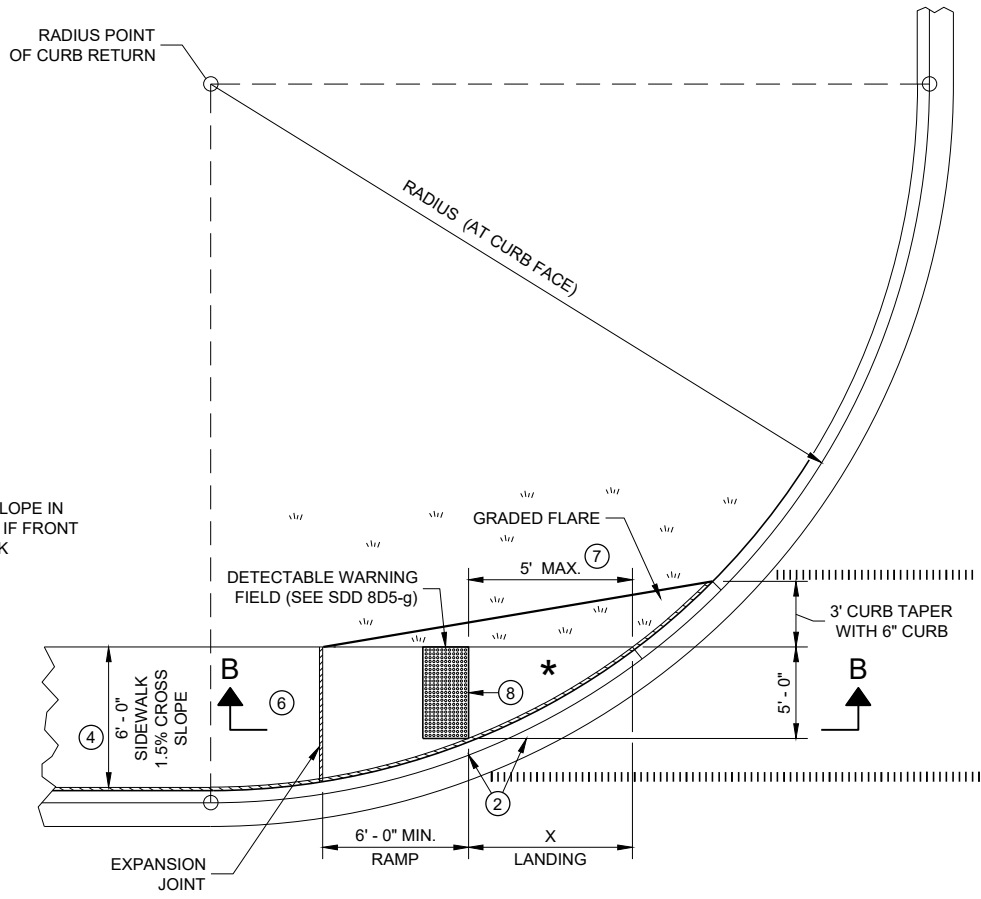
SECTION C - C FOR TYPE 4A

* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IF FRONT OF GRADE BREAK

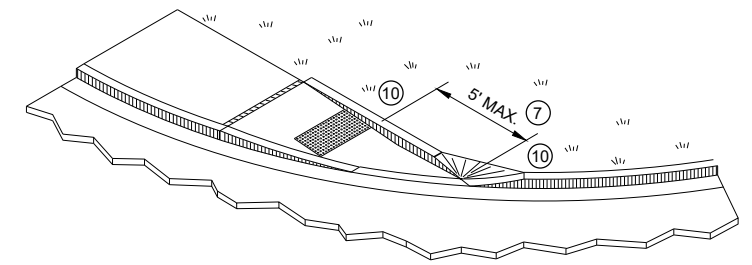


**SECTION B - B FOR
TYPE 4A AND TYPE 4A1**

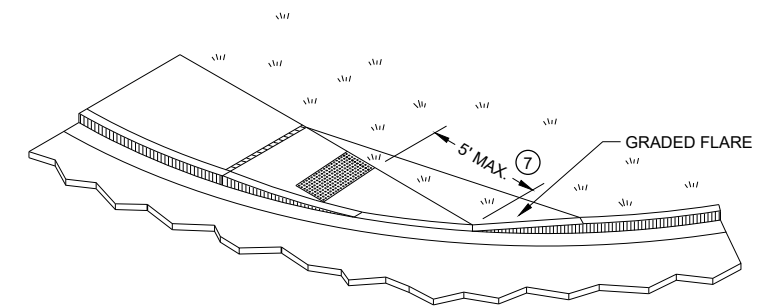
** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED



**PLAN VIEW
CURB RAMP TYPE 4A1**



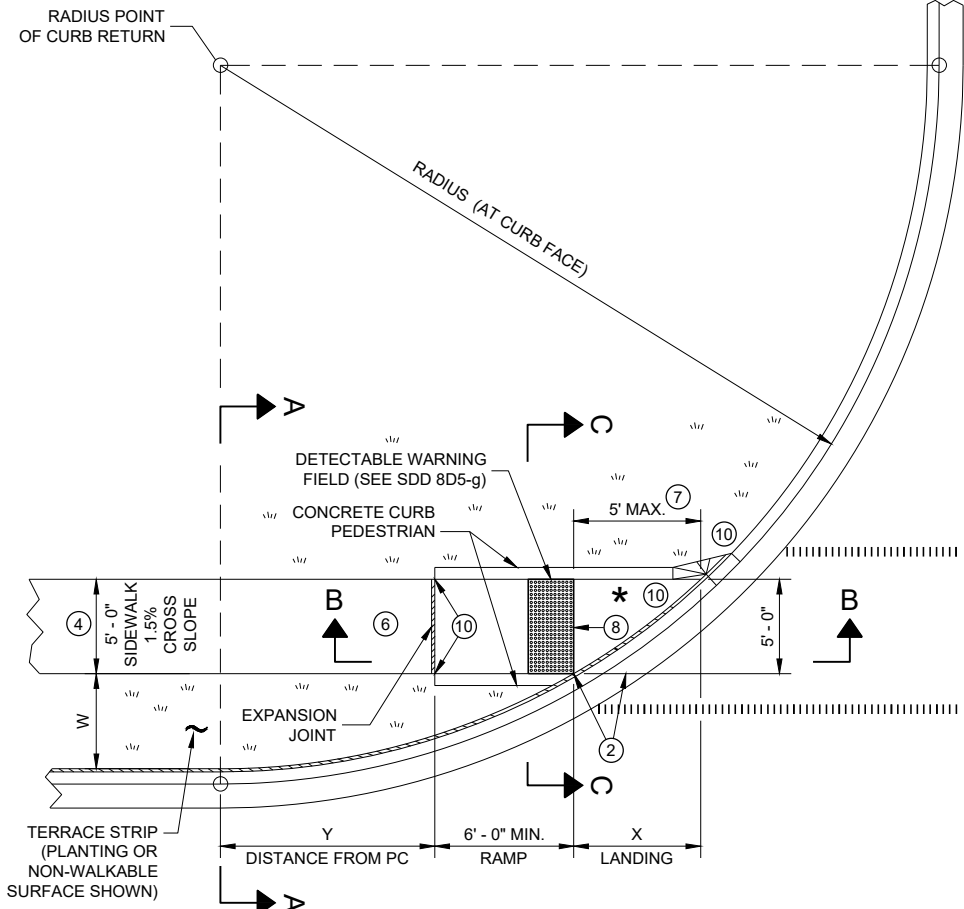
ISOMETRIC VIEW FOR TYPE 4A



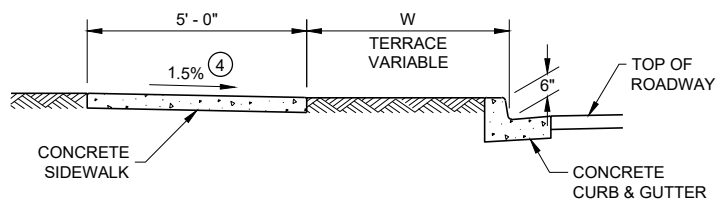
ISOMETRIC VIEW FOR TYPE 4A1

**CURB RAMPS
TYPE 4A AND 4A1**

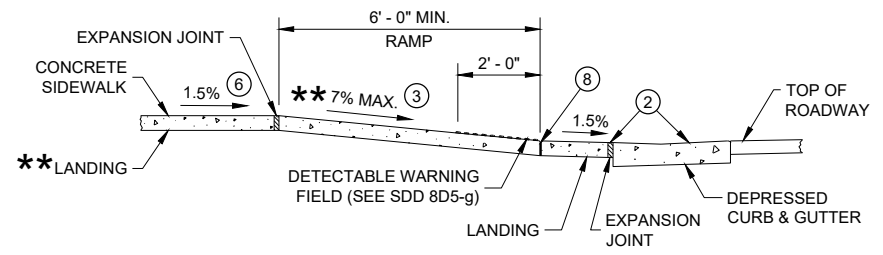
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PLAN VIEW CURB RAMP TYPE 4B



SECTION A - A FOR TYPE 4B



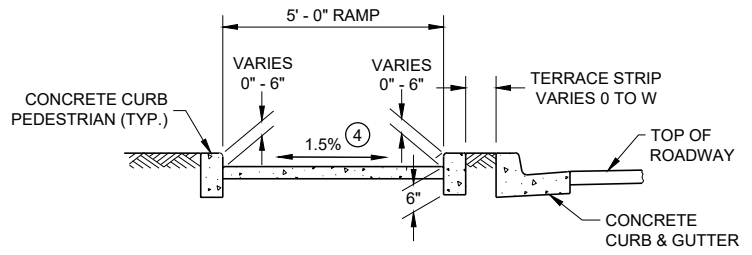
SECTION B - B FOR TYPE 4B AND TYPE 4B1

** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

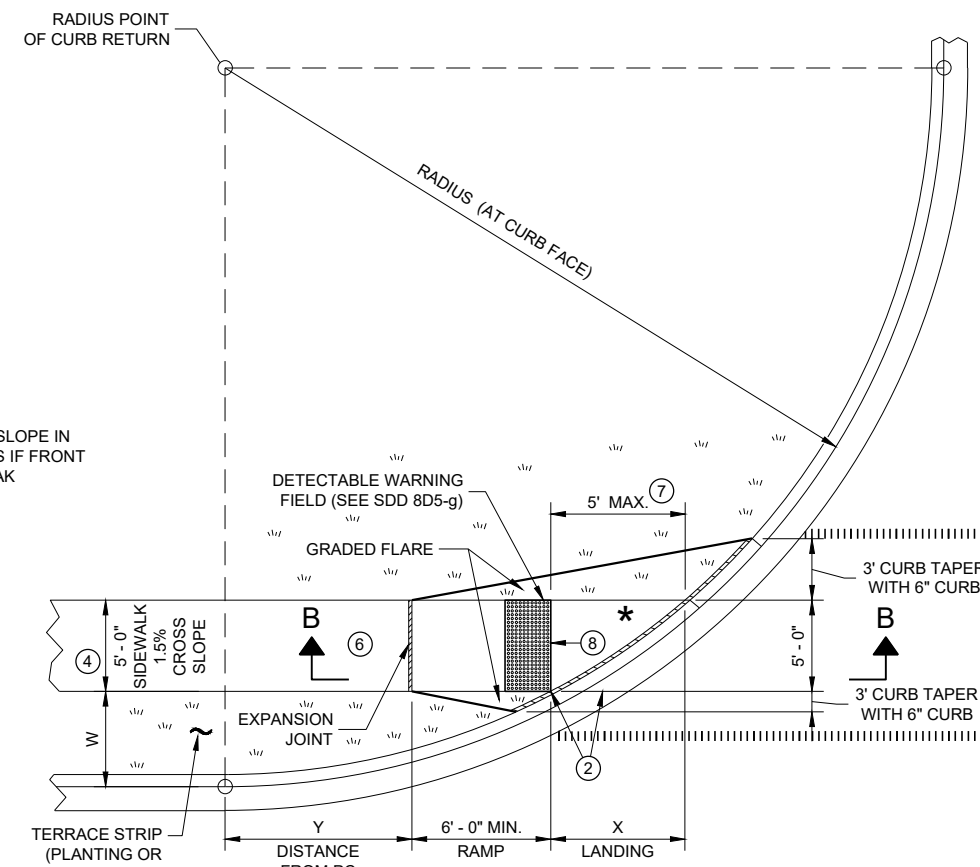
* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IF FRONT OF GRADE BREAK

RADIUS (AT CURB FACE)	W = 3' - 0"		W = 4' - 0"		W = 5' - 0"		W = 6' - 0"		W = 7' - 0"		W = 8' - 0"		W = 9' - 0"		W = 10' - 0"	
	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y
10 FEET	2' - 10 1/4"	0' - 5"	2' - 1"	1' - 4 1/2"	1' - 5"	2' - 1"	0' - 10"	2' - 7 1/2"	0' - 3 1/4"	3' - 0 1/4"						
15 FEET	4' - 6 3/4"	2' - 1 3/4"	3' - 9"	3' - 5 3/4"	3' - 1 1/4"	4' - 6"	2' - 6 3/4"	5' - 4 1/2"	2' - 1"	6' - 1"	1' - 8"	6' - 8 1/2"	1' - 3 1/4"	7' - 2 1/2"	0' - 10 3/4"	7' - 7 1/4"
20 FEET	5' - 9 3/4"	3' - 6 1/2"	4' - 11 1/2"	5' - 1 3/4"	4' - 3 1/4"	6' - 5 1/2"	3' - 8 3/4"	7' - 7"	3' - 3"	8' - 6 1/2"	2' - 10"	9' - 4 1/2"	2' - 5 1/2"	10' - 1 1/4"	2' - 1 1/4"	10' - 9"
30 FEET			6' - 9 1/4"	7' - 11 1/4"	6' - 0 1/4"	9' - 8"	5' - 5"	11' - 1 3/4"	4' - 10 3/4"	12' - 5 3/4"	4' - 5 1/2"	13' - 7 3/4"	4' - 0 3/4"	14' - 8 1/2"	3' - 8 1/2"	15' - 8 1/4"
40 FEET									6' - 1 3/4"	15' - 8 1/2"	5' - 8"	17' - 2"	5' - 3"	18' - 5 3/4"	4' - 10 3/4"	19' - 8 1/4"
50 FEET															5' - 10 1/4"	23' - 2"

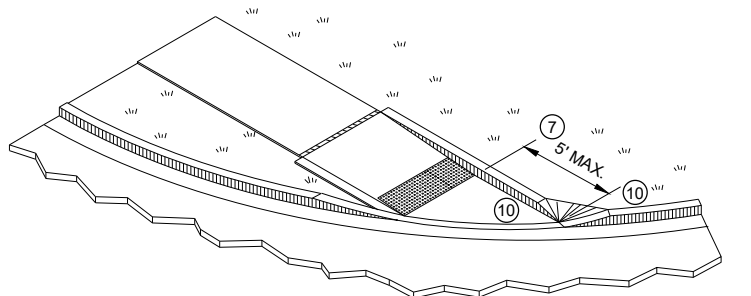
INTERMEDIATE RADII CAN BE INTERPOLATED
 DIMENSION "Y" IS CALCULATED BASED ON 6'-0" RAMP LENGTH
 DIMENSION "X" IS CALCULATED BASED ON 5'-0" SIDEWALK WIDTH



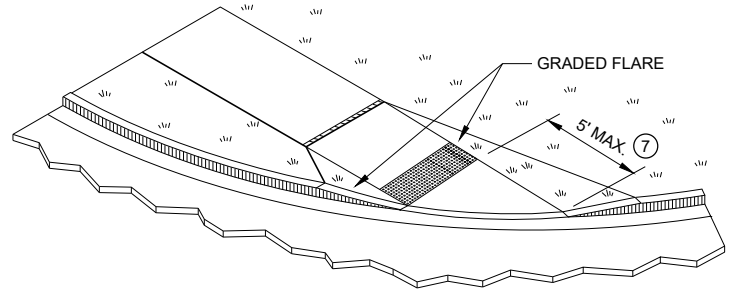
SECTION C - C FOR TYPE 4B



PLAN VIEW CURB RAMP TYPE 4B1



ISOMETRIC VIEW FOR TYPE 4B



ISOMETRIC VIEW FOR TYPE 4B1

LEGEND

- 1/2" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT SIDEWALK
- PAVEMENT MARKING CROSSWALK (WHITE)

GENERAL NOTES

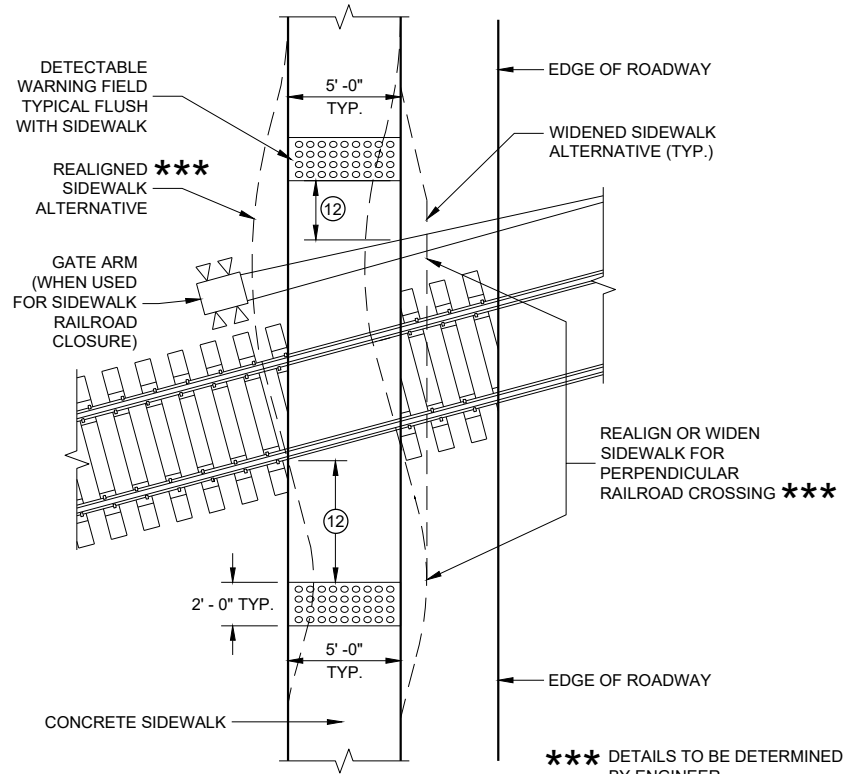
- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/2 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑦ WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑩ INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

SDD 08D05 - 20d

SDD 08D05 - 20d

CURB RAMPS TYPE 4B AND 4B1

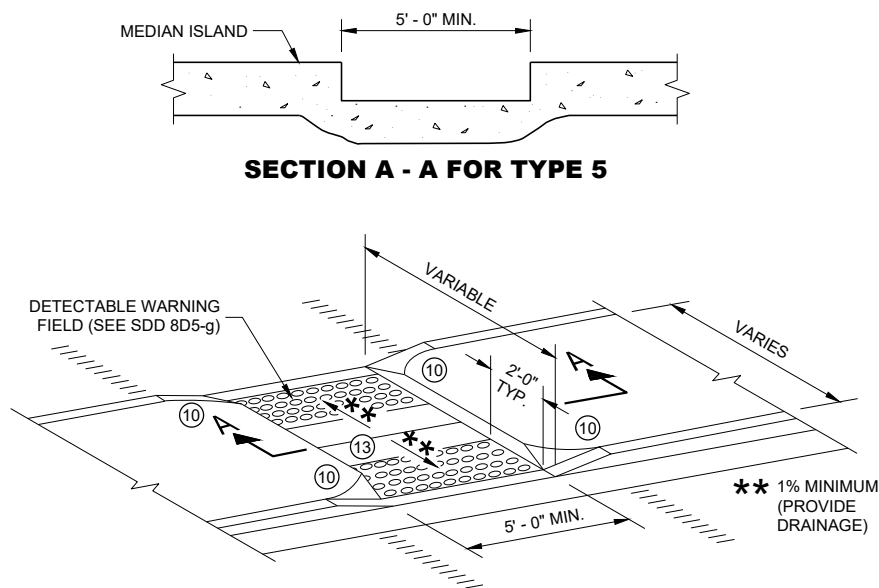
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CURB RAMP TYPE 8

DETECTABLE WARNINGS AT RAILROAD CROSSING

*** DETAILS TO BE DETERMINED BY ENGINEER



SECTION A - A FOR TYPE 5

CURB RAMP TYPE 5

**MEDIAN ISLAND
NON-ELEVATED PEDESTRIAN CROSSING**

GENERAL NOTES

AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

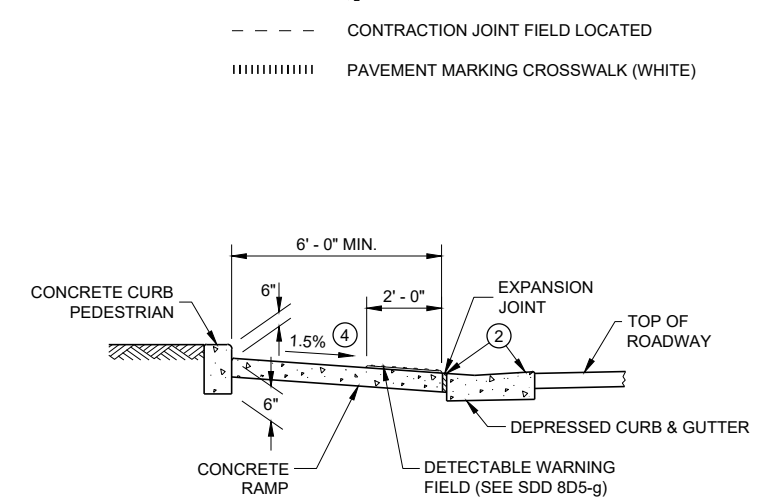
SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.

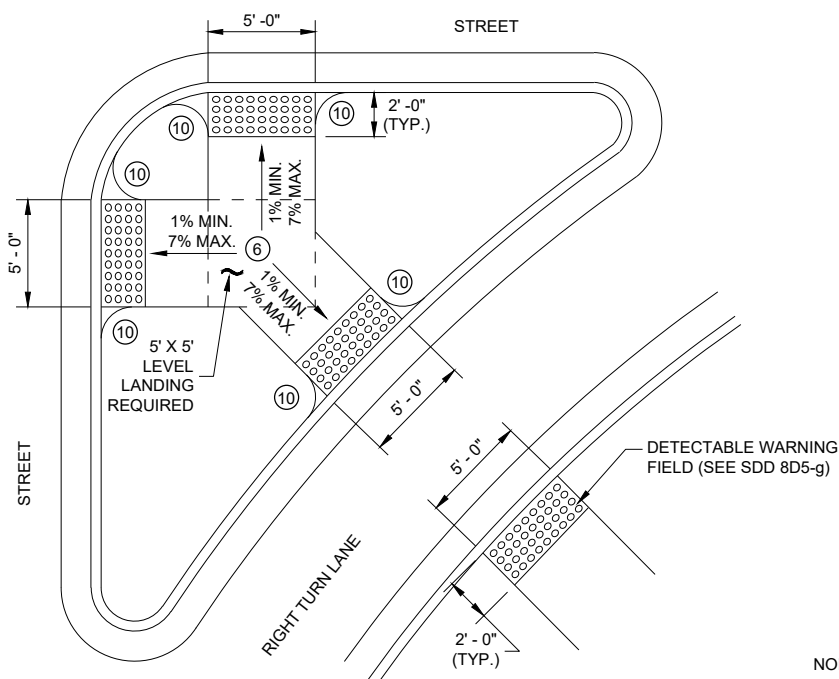
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑩ INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.
- ⑪ SLOPE SIDEWALK TOWARD LANDING AS SHOWN WHERE THERE IS NO TERRACE OR WHERE THE TERRACE WIDTH IS LESS THAN 6 FEET WIDE.
- ⑫ THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO A RAILROAD CROSSING SHALL BE 1.5 FEET ±0.1' FROM THE FACE OF THE GATE ARM IF THE GATE ARM EXTENDS ACROSS THE SIDEWALK. WHERE THERE IS NO PEDESTRIAN GATE, THE EDGE OF THE DETECTABLE WARNING FIELD NEAREST TO THE RAILROAD CROSSING SHALL BE 15 FEET FROM THE NEAREST RAIL.
- ⑬ DO NOT INSTALL DETECTABLE WARNING FIELDS AT THE EDGES OF STREET-LEVEL PEDESTRIAN REFUGE ISLANDS IF A MINIMUM 2 FOOT CONCRETE SURFACE WITHOUT DETECTABLE WARNINGS (MEASURED IN THE DIRECTION OF PEDESTRIAN TRAVEL) CANNOT BE ACHIEVED.

LEGEND

- ===== 1/2" EXPANSION JOINT SIDEWALK
- - - - - CONTRACTION JOINT FIELD LOCATED
- ||||||| PAVEMENT MARKING CROSSWALK (WHITE)



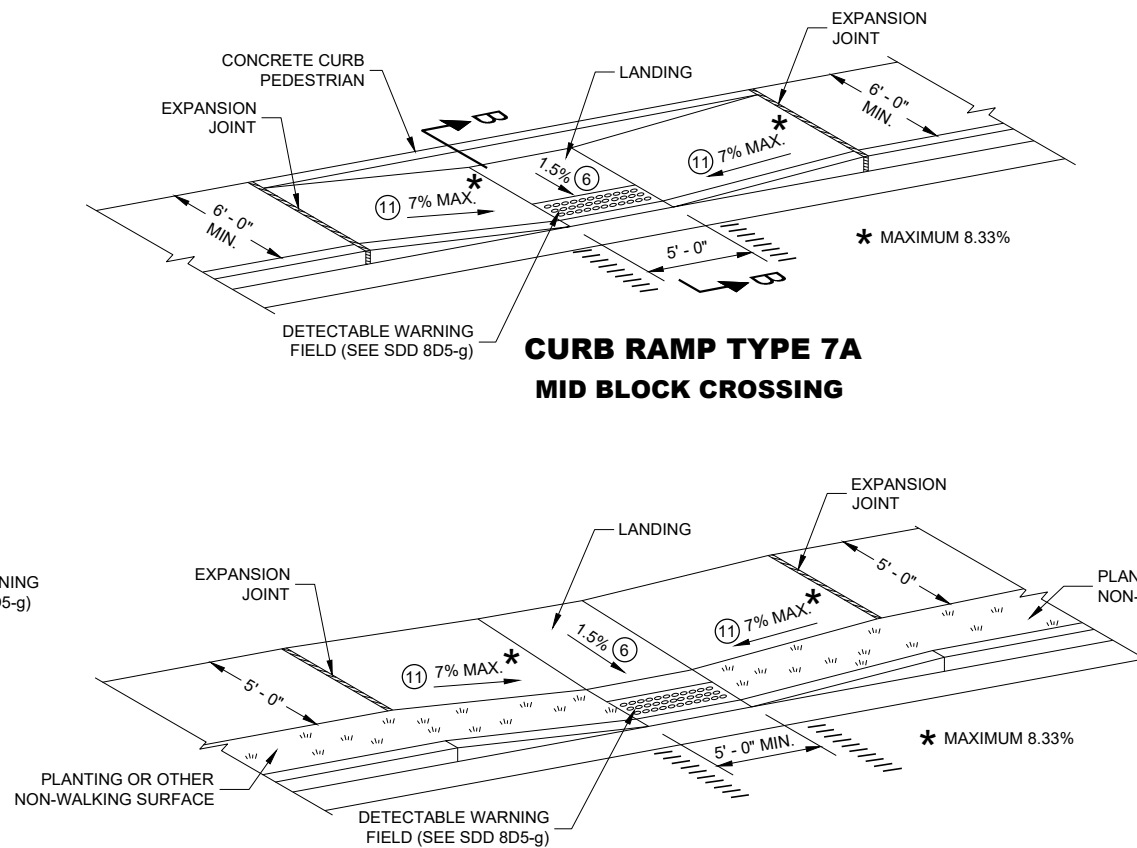
SECTION B - B FOR TYPE 7A



CURB RAMP TYPE 6

DETECTABLE WARNING AT ISLANDS

REFER TO GENERAL NOTES ② AND ③ FOR ALL ISLAND CURB RAMPS



**CURB RAMP TYPE 7A
MID BLOCK CROSSING**

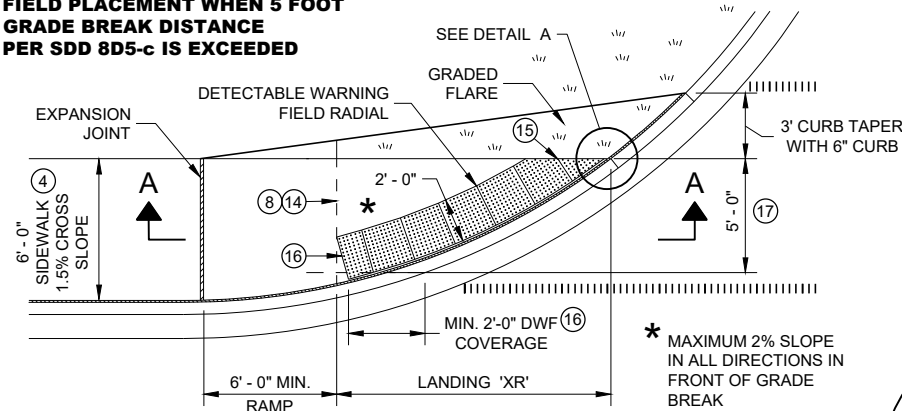
**CURB RAMP TYPE 7B
MID BLOCK CROSSING**

NOTE: THESE PARALLEL AND PARALLEL/PERPENDICULAR CURB RAMPS MAY BE USED AT INTERSECTIONS AND MID BLOCK LOCATIONS.

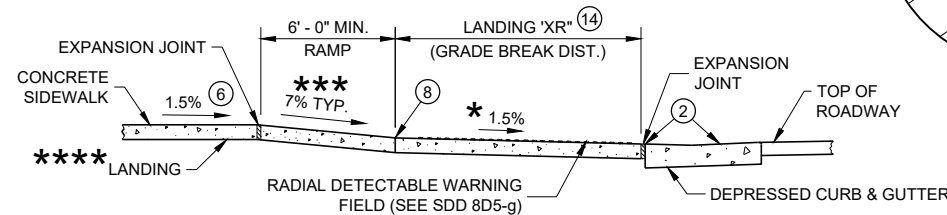
**CURB RAMPS
TYPE 5, 6, 7A, 7B & 8**

STATE OF WISCONSIN
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RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-c IS EXCEEDED



PLAN VIEW CURB RAMP TYPE 4A1 (GRADE BREAK DISTANCE GREATER THAN 5 FEET)

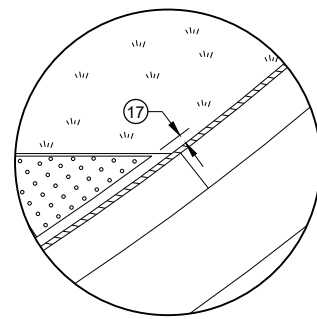


SECTION A - A FOR TYPE 4A1

**** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

*** MAXIMUM 8.33%

- LEGEND**
- 1/2" EXPANSION JOINT SIDEWALK
 - - - - - CONTRACTION JOINT SIDEWALK
 - ||||| PAVEMENT MARKING CROSSWALK (WHITE)

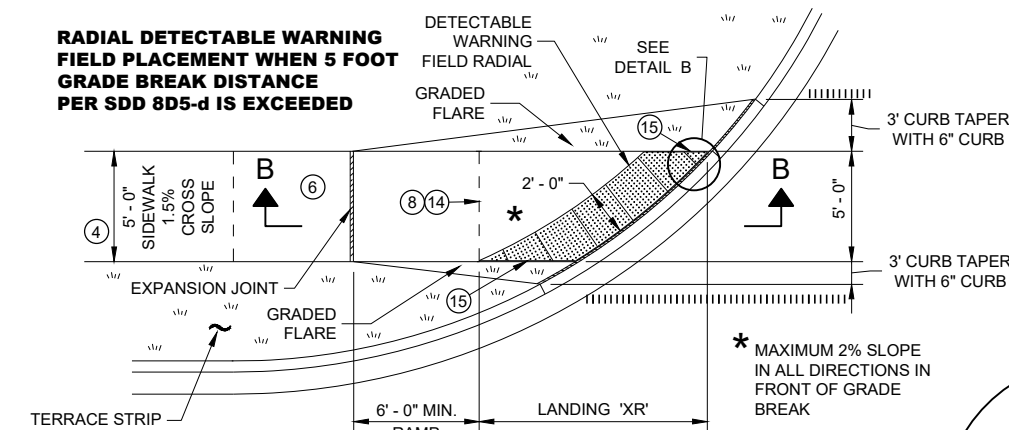


DETAIL A

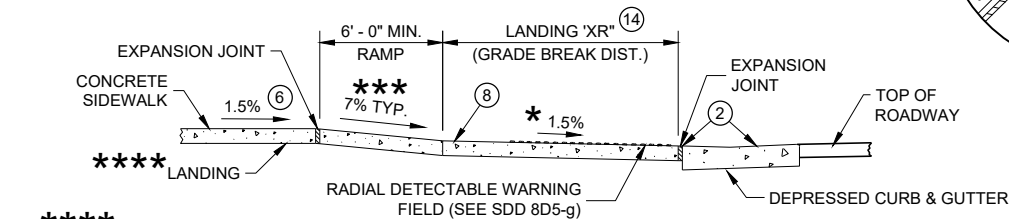
GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- APPLY RADIAL DETECTABLE WARNING PLACEMENT SIMILARLY FOR TYPE 4A AND 4A1 CURB RAMPS AND SIMILARLY FOR TYPE 4B AND 4B1 CURB RAMPS. TYPE 4A AND 4B RAMPS ARE NOT SHOWN.
- REFER TO SDD 8D5-g FOR ADDITIONAL RADIAL PLATE REQUIREMENTS.
- FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FILED ARE PROHIBITED.
- DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.
- 2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- 3) AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- 4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- 6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET BY 5 FEET.
- 8) PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- 14) CONSULT ENGINEER IF GRADE BREAK LOCATION (END OF LANDING DIMENSION "XR") REQUIRES FIELD ADJUSTMENT WHEN ESTABLISHING FINAL RADIAL DETECTABLE WARNING FIELD LOCATION.
- 15) FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN 1/8" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.
- 16) USE 1' X 2" RECTANGULAR END PLATE AT END OF TYPE 4A1 RAMP AND PROVIDE MINIMUM 2' - 0" DETECTABLE WARNING FIELD COVERAGE (IN DIRECTION OF PEDESTRIAN TRAVEL) ALONG THE ENTIRE CURB RAMP WIDTH.
- 17) A MAXIMUM 3 INCH CONCRETE BORDER WITH IS ALLOWABLE IN FROM OF RADIAL DETECTABLE WARNING FIELD FOR CONSTRUCTABILITY PURPOSES. CONCRETE BORDER WIDTH MAY VARY UP TO 1 INCH.

RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-d IS EXCEEDED



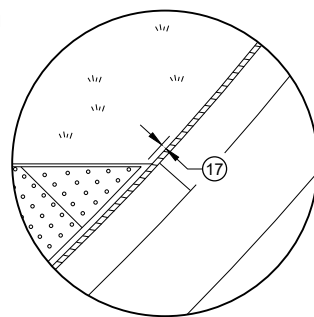
PLAN VIEW CURB RAMP TYPE 4B1 (GRADE BREAK DISTANCE GREATER THAN 5 FEET)



SECTION B - B FOR TYPE 4B1

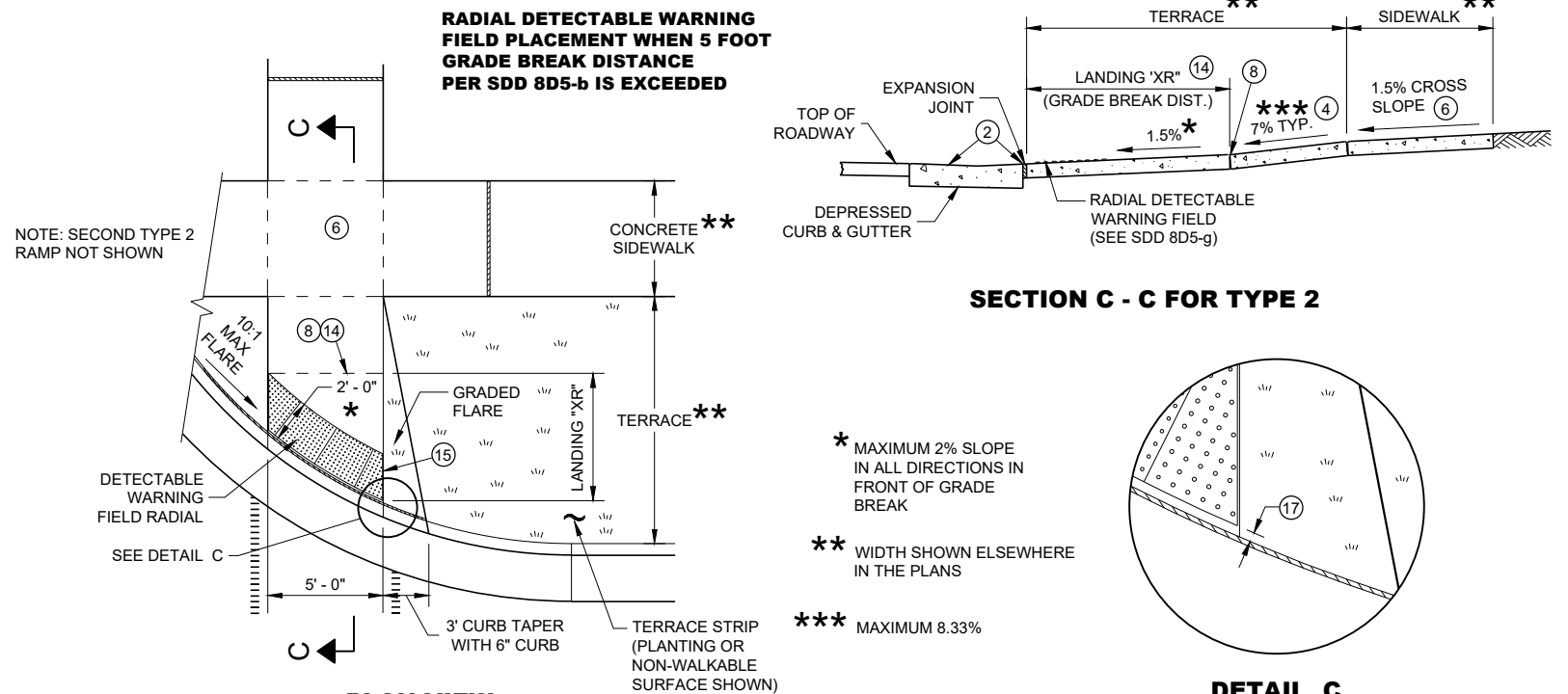
**** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

*** MAXIMUM 8.33%



DETAIL B

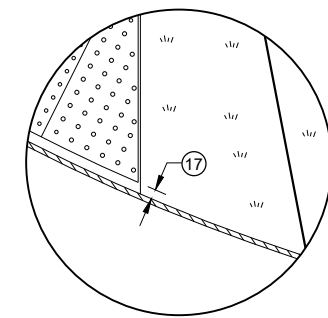
RADIAL DETECTABLE WARNING FIELD PLACEMENT WHEN 5 FOOT GRADE BREAK DISTANCE PER SDD 8D5-b IS EXCEEDED



PLAN VIEW CURB RAMP TYPE 2 (GRADE BREAK DISTANCE GREATER THAN 5 FEET) (ON LINE WITH SIDEWALK)

NOTE: SECOND TYPE 2 RAMP NOT SHOWN

- * MAXIMUM 2% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK
- ** WIDTH SHOWN ELSEWHERE IN THE PLANS
- *** MAXIMUM 8.33%



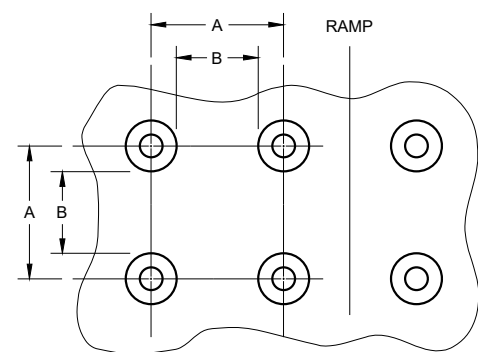
DETAIL C

CURB RAMPS RADIAL DETECTABLE WARNING FIELD APPLICATIONS

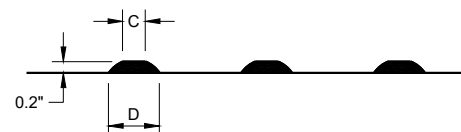
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

	MIN.	MAX.
A	1.6"	2.4"
B	0.65"	1.5"
C	*	*
D	0.9"	1.4"

* THE C DIMENSION IS 50% TO 65% OF THE D DIMENSION.

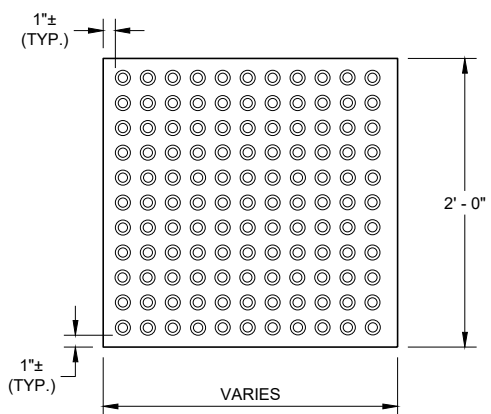


PLAN VIEW

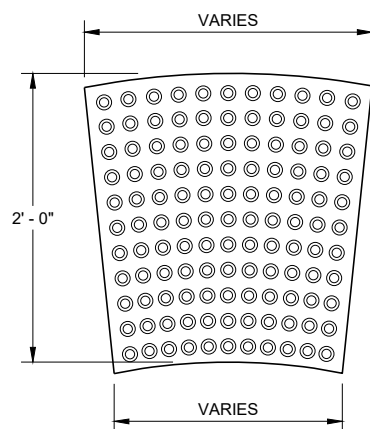


ELEVATION VIEW

**TRUNCATED DOMES
DETECTABLE WARNING PATTERN DETAIL**

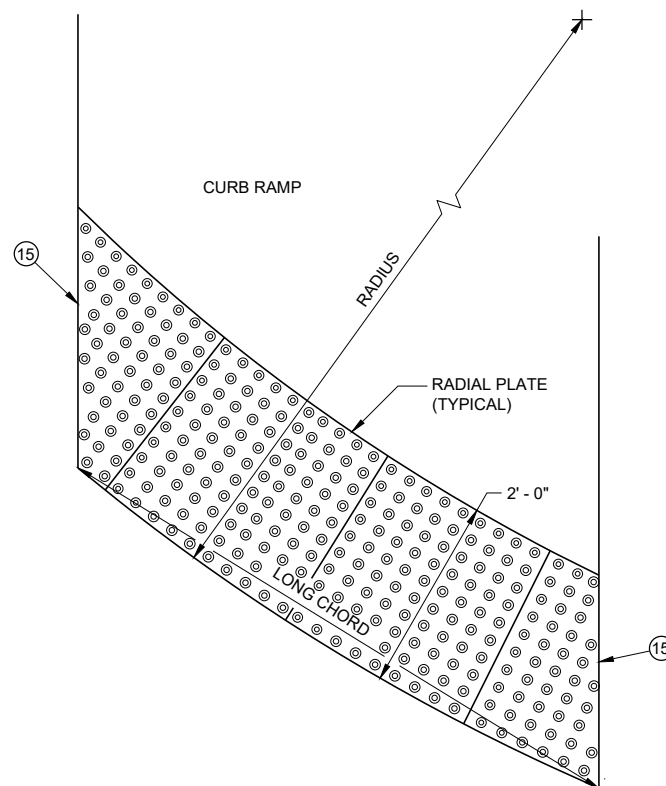


RECTANGULAR
PLATES

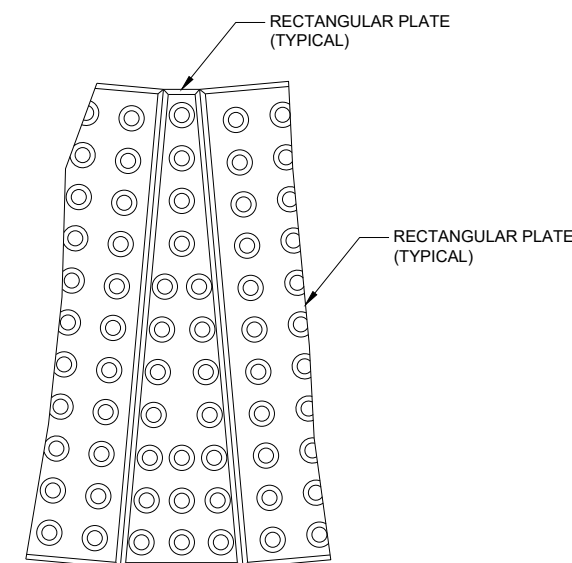


RADIAL
PLATES

PLAN VIEW
DETECTABLE WARNING FIELDS (TYPICAL)



PLAN VIEW
RADIAL DETECTABLE
WARNING FIELD ATTRIBUTES



PLAN VIEW
RADIAL WEDGE PLATE
CONNECTION DETAIL

GENERAL NOTES

DETECTABLE WARNING FIELDS THAT ARE INSTALLED AT A CURB RAMP SHALL BE FROM THE SAME MANUFACTURER.

PLACE ALL DETECTABLE WARNING FIELD SYSTEMS IN ACCORDANCE TO THE MANUFACTURER'S RECOMMENDATION.

FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FIELD ARE PROHIBITED.

DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.

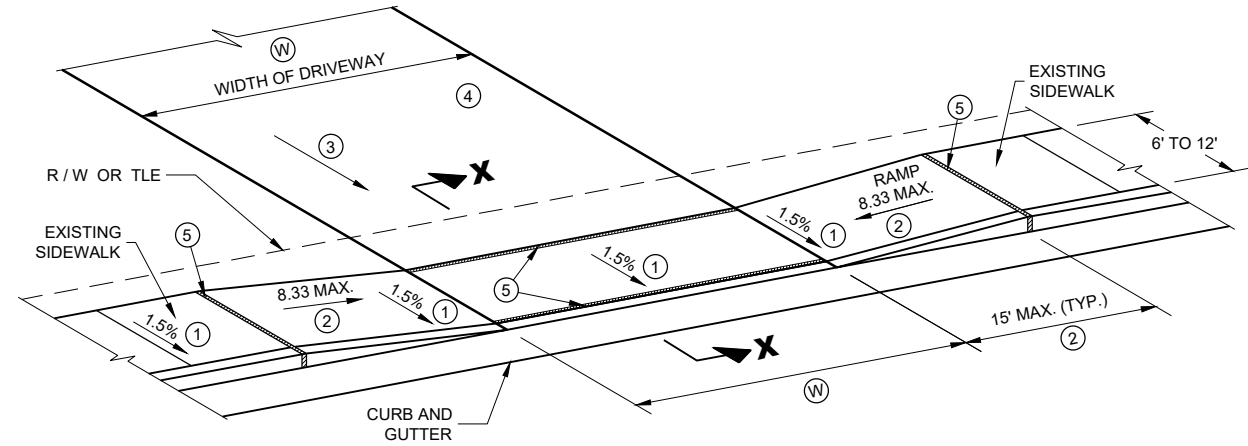
FOR RADIAL DETECTABLE WARNING FIELD APPLICATIONS WHERE STANDARD RADIAL PLATES ARE NOT AVAILABLE AT AN INTERSECTION CURB RADIUS, A COMBINATION OF SQUARE OR RECTANGULAR PLATES AND RADIAL PLATES MAY BE USED TO FORM RADIAL CONFIGURATION. RADIAL WEDGE PLATES IN COMBINATION WITH SQUARE PLATES ARE ALSO ACCEPTABLE. FOLLOW MANUFACTURER'S RECOMMENDATIONS.

REFER TO CONTRACT AND STANDARD SPECIFICATIONS FOR FIELD CUTTING REQUIREMENTS.

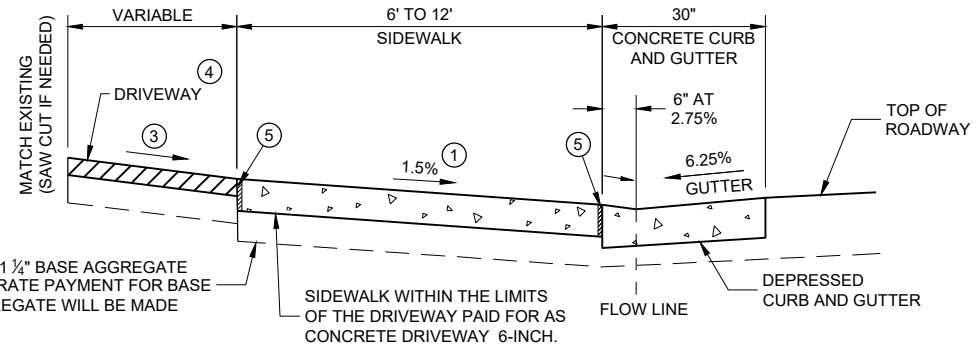
DO NOT EMBED IN CONCRETE ANY FIELD-CUT PLATES WITH CUT EDGES SHORTER THAN 6 INCHES. CONSULT WITH MANUFACTURER FOR RE-DRILLING AND ANCHORING REQUIREMENTS OF FIELD-CUT PLATES.

15 FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN 1/8" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.

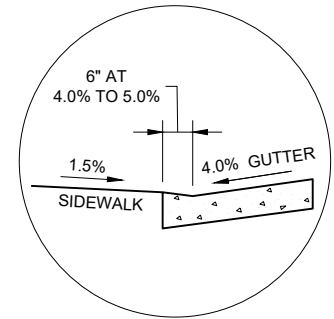
CURB RAMPS RECTANGULAR AND RADIAL DETECTABLE WARNING PLATES	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2019 DATE	/S/ Rodney Taylor ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR
<small>FHWA</small>	



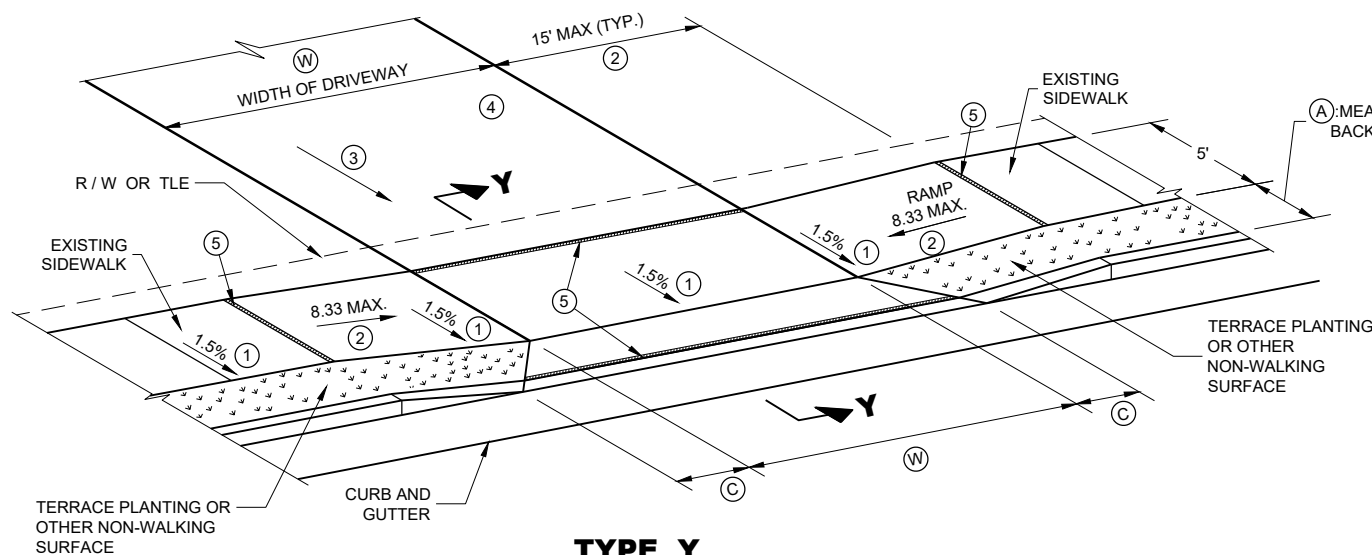
TYPE X
SIDWALK ABUTS CURB AND GUTTER
TERRACE VARIES 0 TO 3 FEET



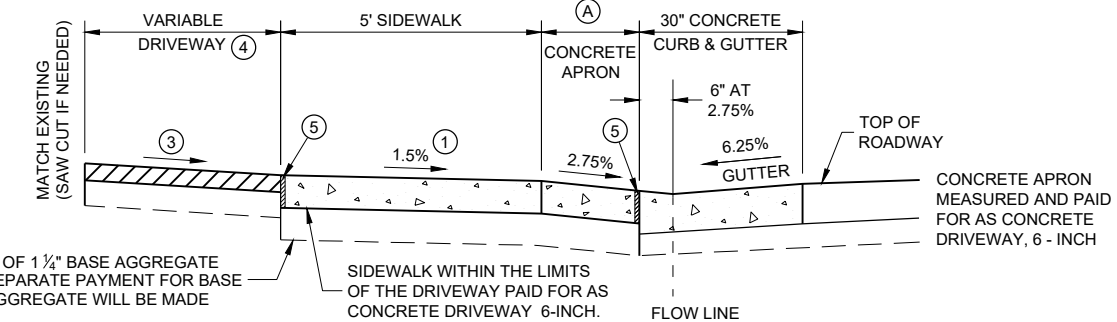
SECTION X - X



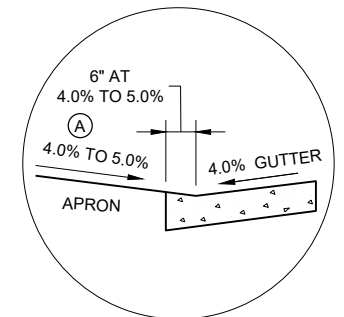
SECTION X - X
4% GUTTER SLOPE



TYPE Y
SIDWALK WITH NARROWER TERRACE
TERRACE VARIES 4 TO 6 FEET



SECTION Y - Y
DRIVEWAY DETAIL WITH CONCRETE
CURB AND GUTTER
(URBAN AND SUBURBAN)



SECTION Y - Y
4% GUTTER SLOPE

(W): 12' MIN. - 24' MAX. RESIDENTIAL AND NON-COMMERCIAL (PE & FE)
 16' MIN. - 35' MAX. COMMERCIAL (CE)

TABLE Y

(A) FEET	(C) FEET
3.5'	2.0'
4.5'	3.0'
5.5'	3.5'

(A): MEASURE FROM BACK OF CURB

6" OF 1 1/4" BASE AGGREGATE SEPARATE PAYMENT FOR BASE AGGREGATE WILL BE MADE

NOTE: SIDEWALK MAY BE DEPRESSED IN DRIVEWAY AREAS

GENERAL NOTES

PROVIDE CONSTRUCTION JOINTS ALONG THE CENTER OF THE CONCRETE FOR DRIVEWAYS UNDER 20 FEET IN WIDTH AND AT THE THIRD POINTS OVER 20 FEET IN WIDTH.

(W) IS SHOWN ON PLAN AND PROFILE SHEETS.

OFFSETS, ELEVATIONS, AND PERCENT GRADE ARE SHOWN ON THE CROSS SECTIONS.

- ① CONSTRUCTION TOLERANCE OF 0.5%± FOR SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.
- ② THE SIDEWALK RAMP MAXIMUM RUNNING SLOPE SHALL NOT REQUIRE THE RAMP LENGTH TO EXCEED 15 FEET TO AVOID CHASING THE SLOPE INDEFINITELY WHEN CONNECTING TO STEEP GRADES. WHEN APPLYING THE 15 FOOT MAXIMUM LENGTH, THE RUNNING SLOPE OF THE SIDEWALK SHALL BE AS FLAT AS FEASIBLE AND NOT EXCEED THE LONGITUDINAL GRADE OF THE ROADWAY. SLOPE SIDEWALK RAMP TOWARD APRON AS SHOWN WHERE THERE IS NO TERRACE OR WHERE THE TERRACE WIDTH IS LESS THAN 6 FEET WIDE.

- ③ **DRIVEWAY SLOPES: DESIRABLE MAXIMUM**
 10.5% UP AWAY FROM SIDEWALK (SAG)
 8.5% DOWN AWAY FROM SIDEWALK (CREST)
 ABSOLUTE MAXIMUM 15% FOR BOTH CREST AND SAG

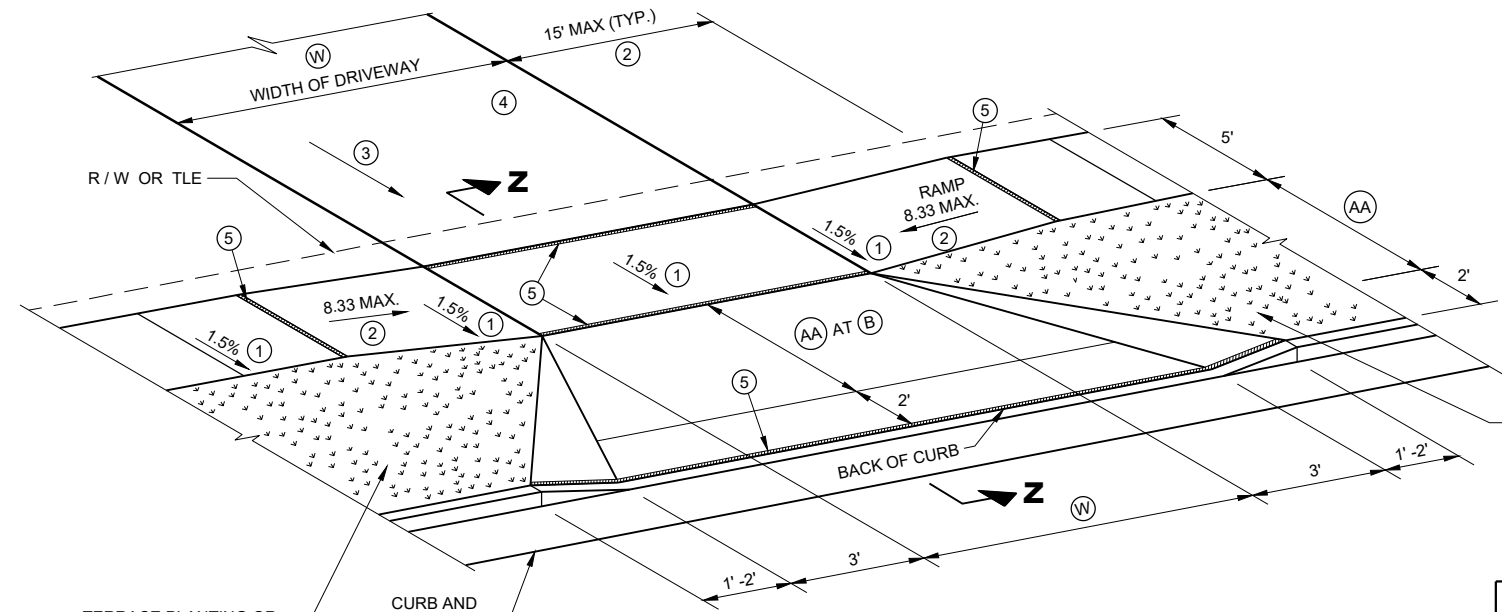
- ④ **DRIVEWAY TYPES**
 • 6-INCH CONCRETE DRIVEWAY PAVEMENT OVER 6-INCH BASE AGGREGATE
 • 2-INCH TO 3-INCH ASPHALTIC SURFACE OVER 6-INCH BASE AGGREGATE
 • 6-INCH BASE AGGREGATE (MAY BE INCREASED FOR CLAY SUBGRADES.)

- ⑤ 1/2" EXPANSION JOINT FILLER

DRIVEWAY AND
SIDWALK RAMPS
TYPES X AND Y

STATE OF WISCONSIN
 DEPARTMENT OF TRANSPORTATION

APPROVED
 February 2022 /S/ Rodney Taylor
 DATE ROADWAY STANDARDS DEVELOPMENT
 ENGINEER



TYPE Z
SIDEWALK WITH WIDER TERRACE
TERRACE VARIES 7 TO 12 FEET

GENERAL NOTES

PROVIDE CONSTRUCTION JOINTS ALONG THE CENTER OF THE CONCRETE FOR DRIVEWAYS UNDER 20 FEET IN WIDTH AND AT THE THIRD POINTS OVER 20 FEET IN WIDTH.

(W) IS SHOWN ON PLAN AND PROFILE SHEETS.

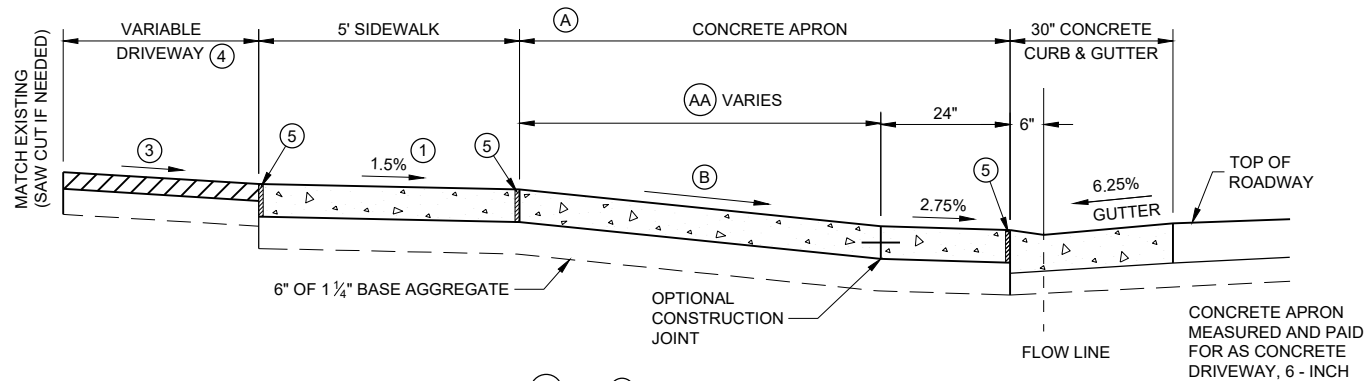
OFFSETS, ELEVATIONS, AND PERCENT GRADE ARE SHOWN ON THE CROSS SECTIONS.

- ① CONSTRUCTION TOLERANCE OF 0.5%± FOR SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2%.
- ② THE SIDEWALK RAMP MAXIMUM RUNNING SLOPE SHALL NOT REQUIRE THE RAMP LENGTH TO EXCEED 15 FEET TO AVOID CHASING THE SLOPE INDEFINITELY WHEN CONNECTING TO STEEP GRADES. WHEN APPLYING THE 15 FOOT MAXIMUM LENGTH, THE RUNNING SLOPE OF THE SIDEWALK SHALL BE AS FLAT AS FEASIBLE AND NOT EXCEED THE LONGITUDINAL GRADE OF THE ROADWAY.
- ③ DRIVEWAY SLOPES: DESIRABLE MAXIMUM
 10.5% UP AWAY FROM SIDEWALK (SAG)
 8.5% DOWN AWAY FROM SIDEWALK (CREST)
 ABSOLUTE MAXIMUM 15% FOR BOTH CREST AND SAG
- ④ DRIVEWAY TYPES
 · 6-INCH CONCRETE DRIVEWAY PAVEMENT OVER 6-INCH BASE AGGREGATE
 · 2-INCH TO 3-INCH ASPHALTIC SURFACE OVER 6-INCH BASE AGGREGATE
 · 6-INCH BASE AGGREGATE (MAY BE INCREASED FOR CLAY SUBGRADES.)
- ⑤ ½" EXPANSION JOINT FILLER.

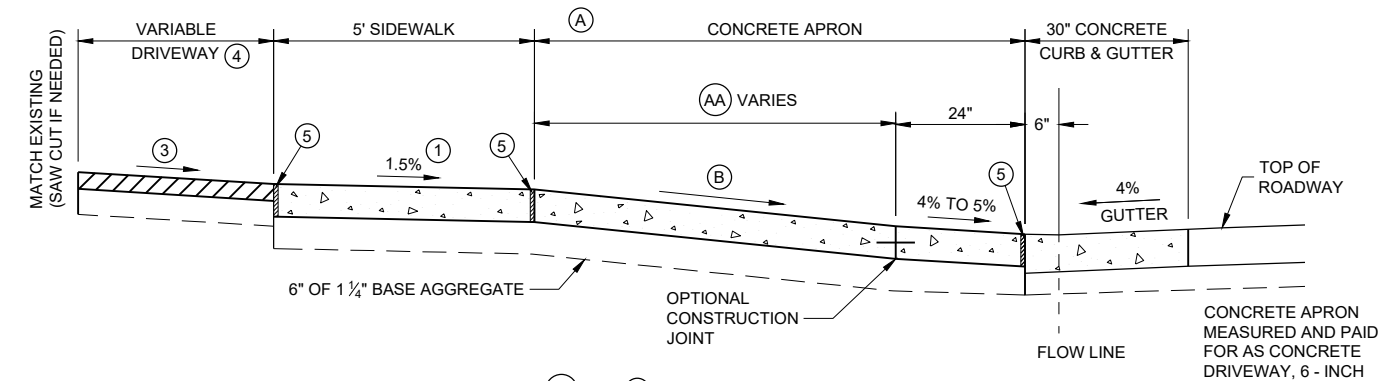
TABLE Z

(AA) FEET	(B) % 6.25% GUTTER	(B) % 4% GUTTER
4.5'	11.5%	9% TO 11.5%
5.5'	9% TO 11.5%	8% TO 11.5%
6.5'	8% TO 11.5%	6% TO 11.5%
7.5'	7% TO 11.5%	6% TO 11.5%
8.5'	6% TO 11.5%	5% TO 11.5%
9.5'	5% TO 11.5%	4% TO 11.5%

(W): 12' MIN. - 24' MAX. RESIDENTIAL AND NON-COMMERCIAL (PE & FE)
 16' MIN. - 35' MAX. COMMERCIAL (CE)



6.25% GUTTER SLOPE



4% GUTTER SLOPE

NOTE: SIDEWALK MAY BE DEPRESSED IN DRIVEWAY AREAS FOR (B) VALUES NOT SHOWN IN TABLE Z.
 SIDEWALK WITHIN THE LIMITS OF THE DRIVEWAY PAID FOR AS CONCRETE DRIVEWAY 6-INCH.
 SEPARATE PAYMENT FOR BASE AGGREGATE WILL BE MADE.

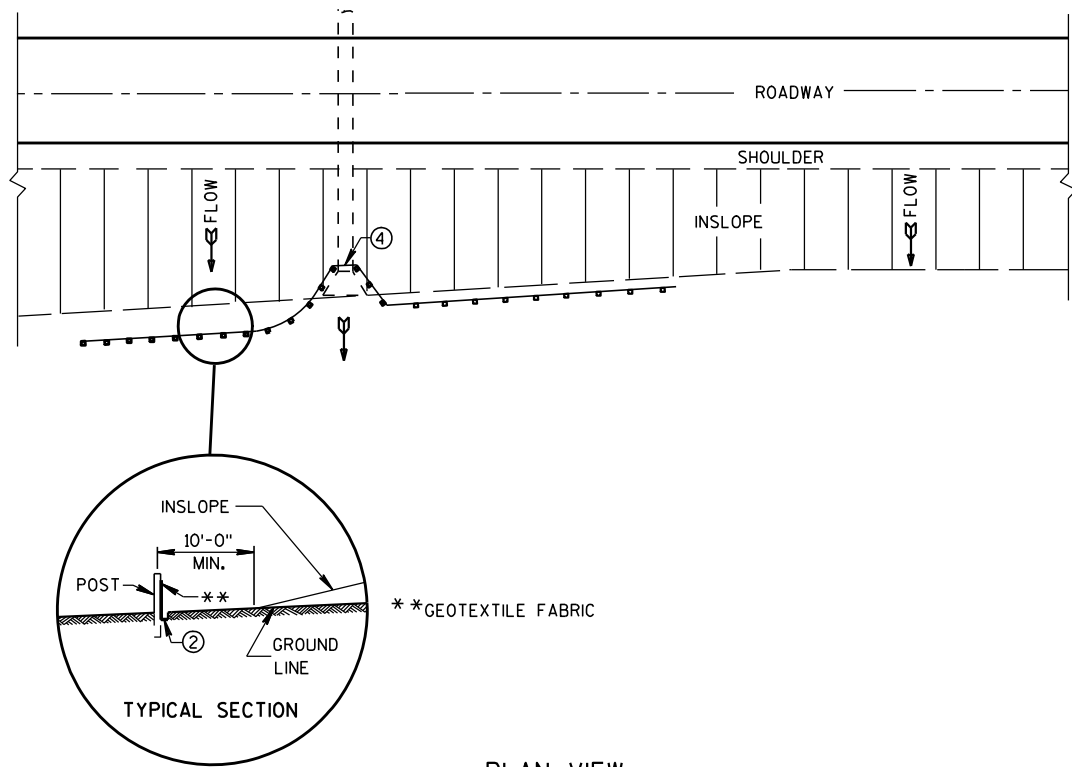
SECTION Z - Z
DRIVEWAY DETAIL WITH CONCRETE CURB AND GUTTER
(URBAN AND SUBURBAN)

DRIVEWAY AND SIDEWALK RAMPS TYPE Z

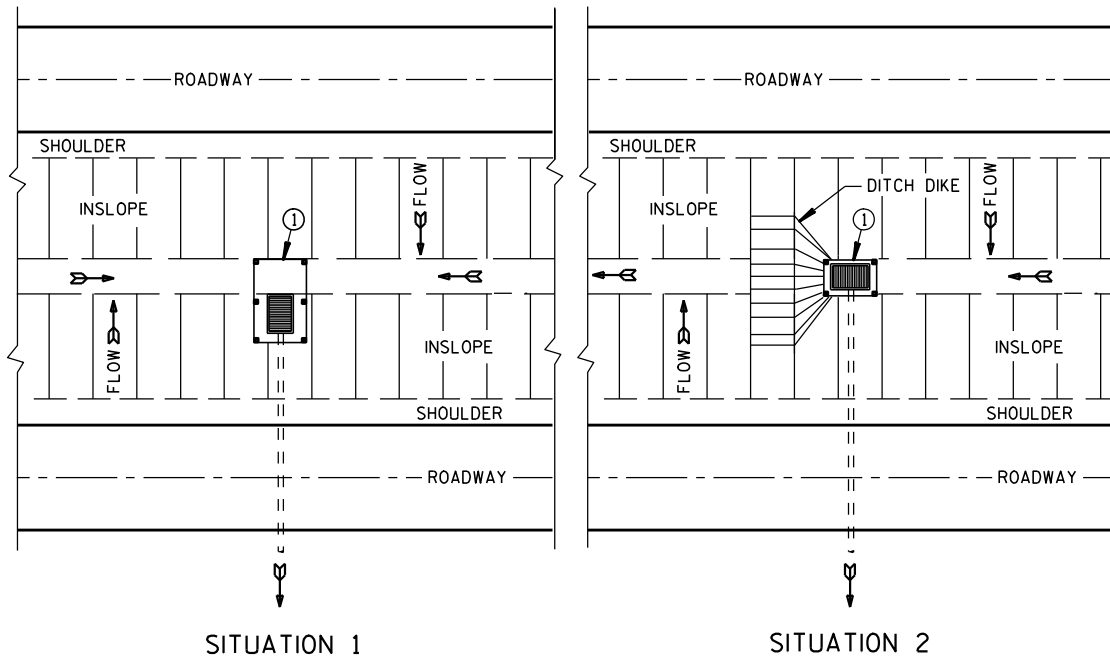
STATE OF WISCONSIN
 DEPARTMENT OF TRANSPORTATION

APPROVED
 February 2022 DATE /S/ Rodney Taylor
 ROADWAY STANDARDS DEVELOPMENT ENGINEER

FHWA



PLAN VIEW
TYPICAL APPLICATION OF SILT FENCE

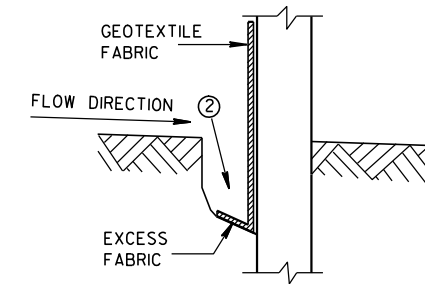


SITUATION 1 SITUATION 2
PLAN VIEW
SILT FENCE AT MEDIAN SURFACE DRAINS

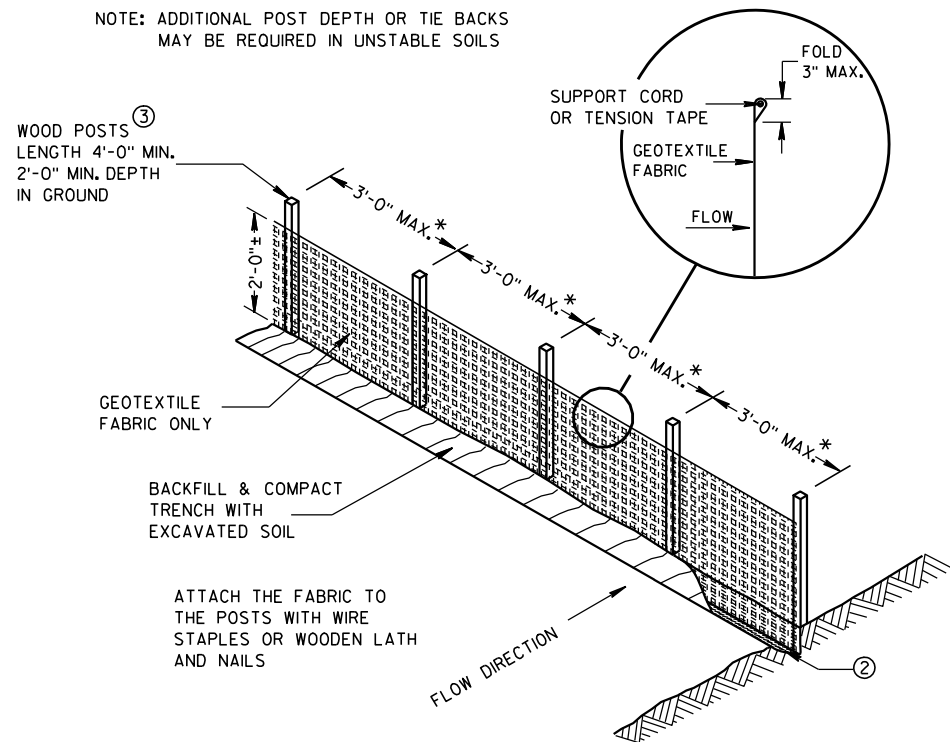
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

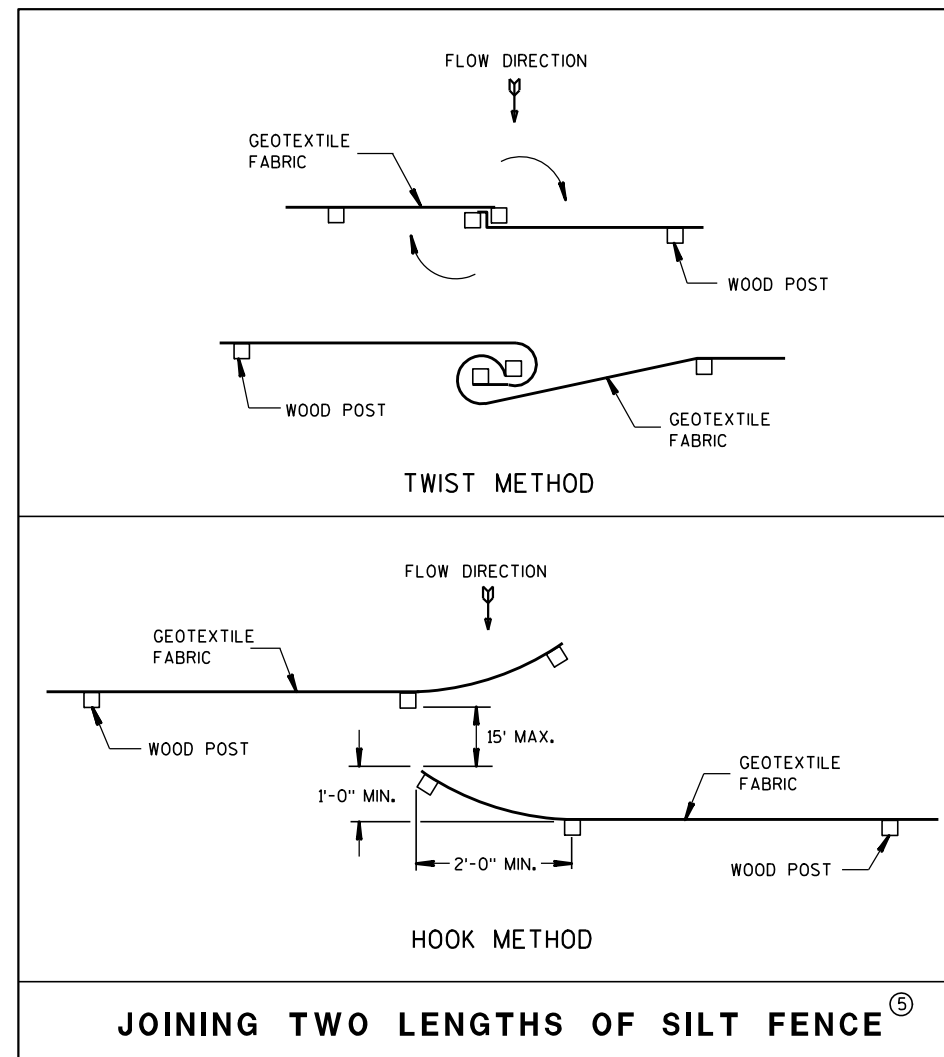
- ① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- ③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1 1/8" X 1 1/8" OF OAK OR HICKORY.
- ④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- ⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



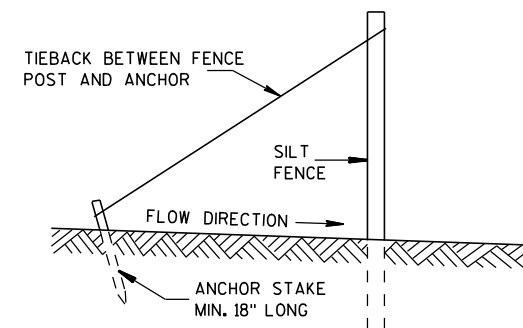
TRENCH DETAIL



SILT FENCE

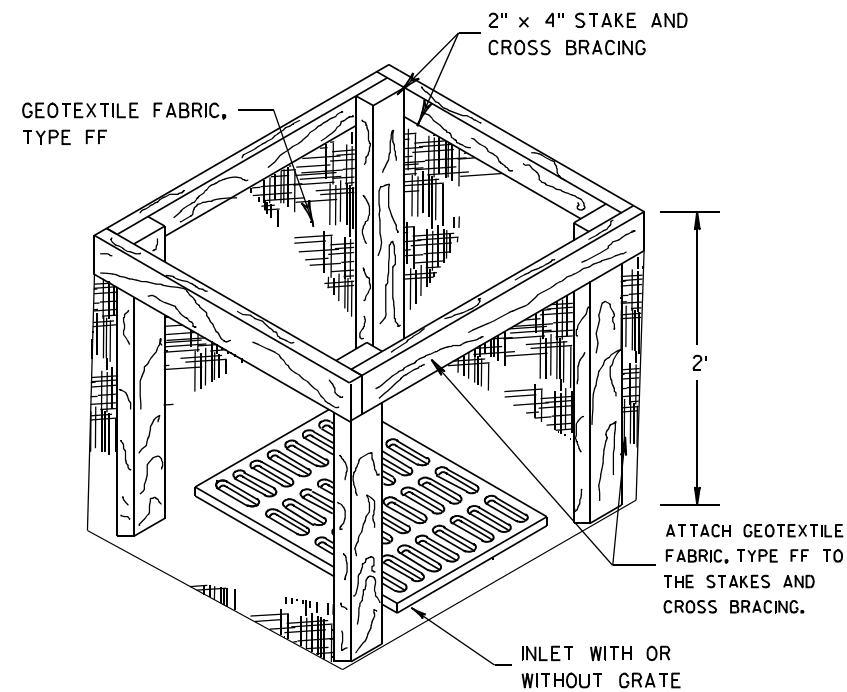
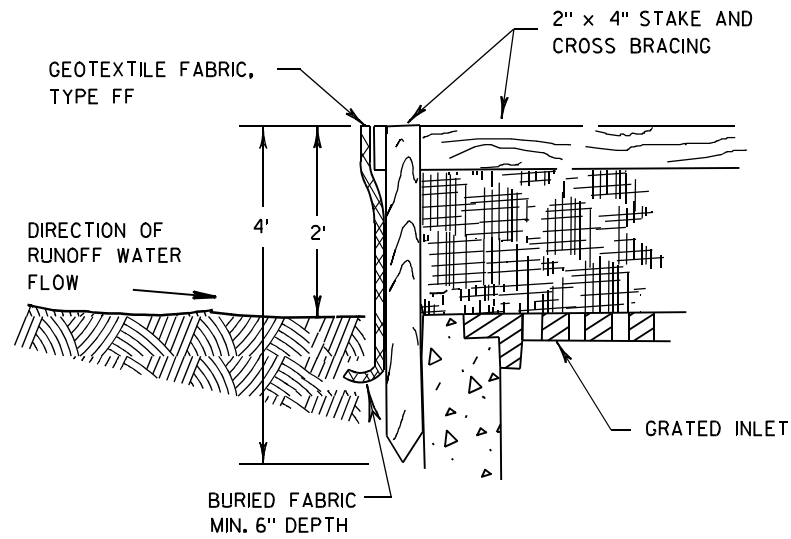


JOINING TWO LENGTHS OF SILT FENCE ⑤



SILT FENCE TIE BACK
(WHEN REQUIRED BY THE ENGINEER)

SILT FENCE	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 4-29-05 DATE	/S/ Beth Canestra CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA	



INLET PROTECTION, TYPE A

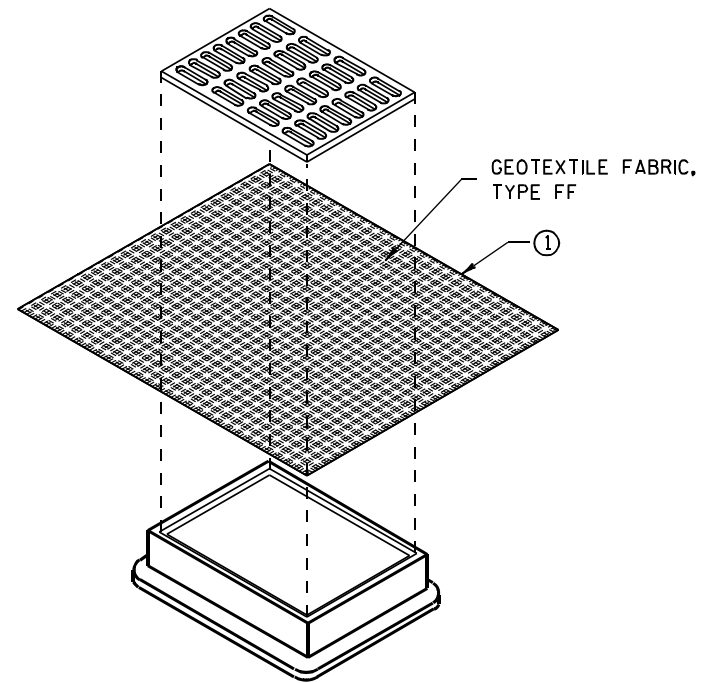
GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

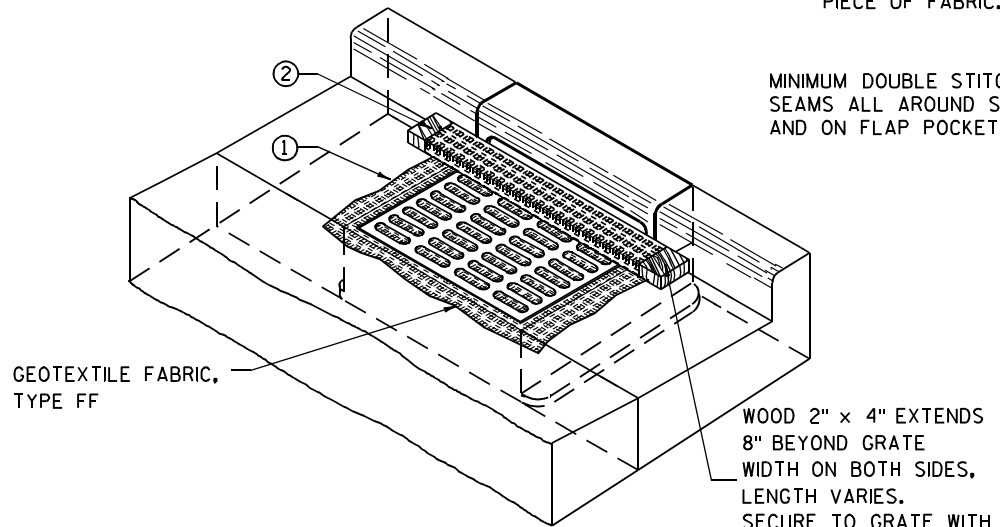
WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- ① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- ② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- ③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



**INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)**

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE C (WITH CURB BOX)

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

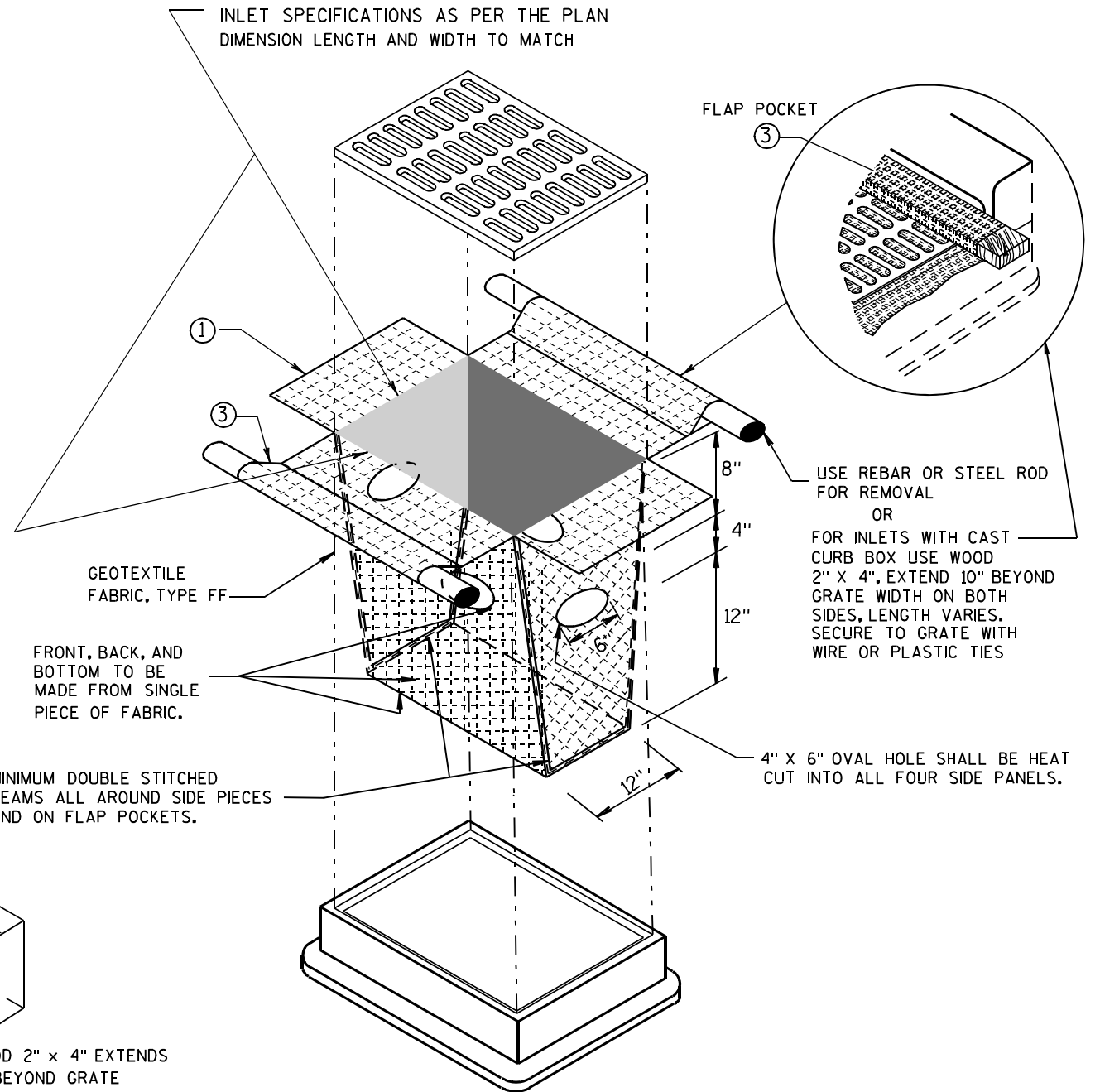
THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.



INLET PROTECTION, TYPE D

(CAN BE INSTALLED IN ANY INLET TYPE WITH OR WITHOUT A CURB BOX AS PER NOTE ②)

INLET PROTECTION TYPE A, B, C, AND D	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 10/16/02 DATE	/s/ Beth Connestra CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA	

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

TRACKING PAD SHALL BE INSPECTED DAILY. DEFICIENT AREAS SHALL BE REPAIRED OR REPLACED IMMEDIATELY.

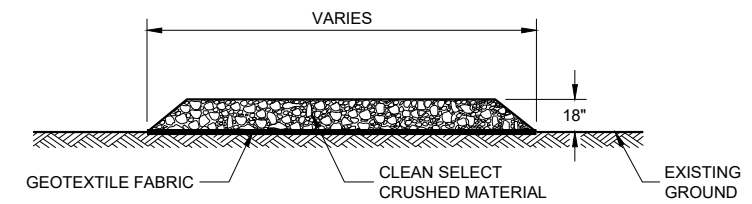
TRACKING PAD TO BE REMOVED AFTER CONSTRUCTION IS COMPLETED.

TRACKING PAD SHALL BE THE FULL WIDTH OF THE EGRESS POINT.

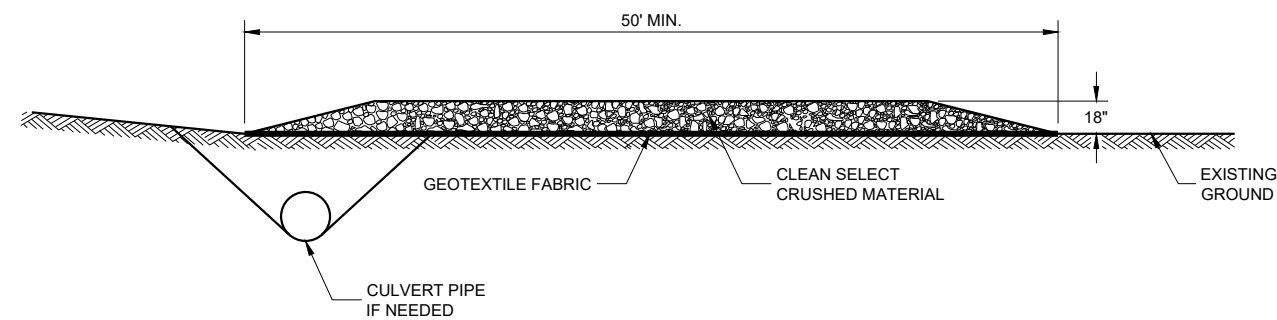
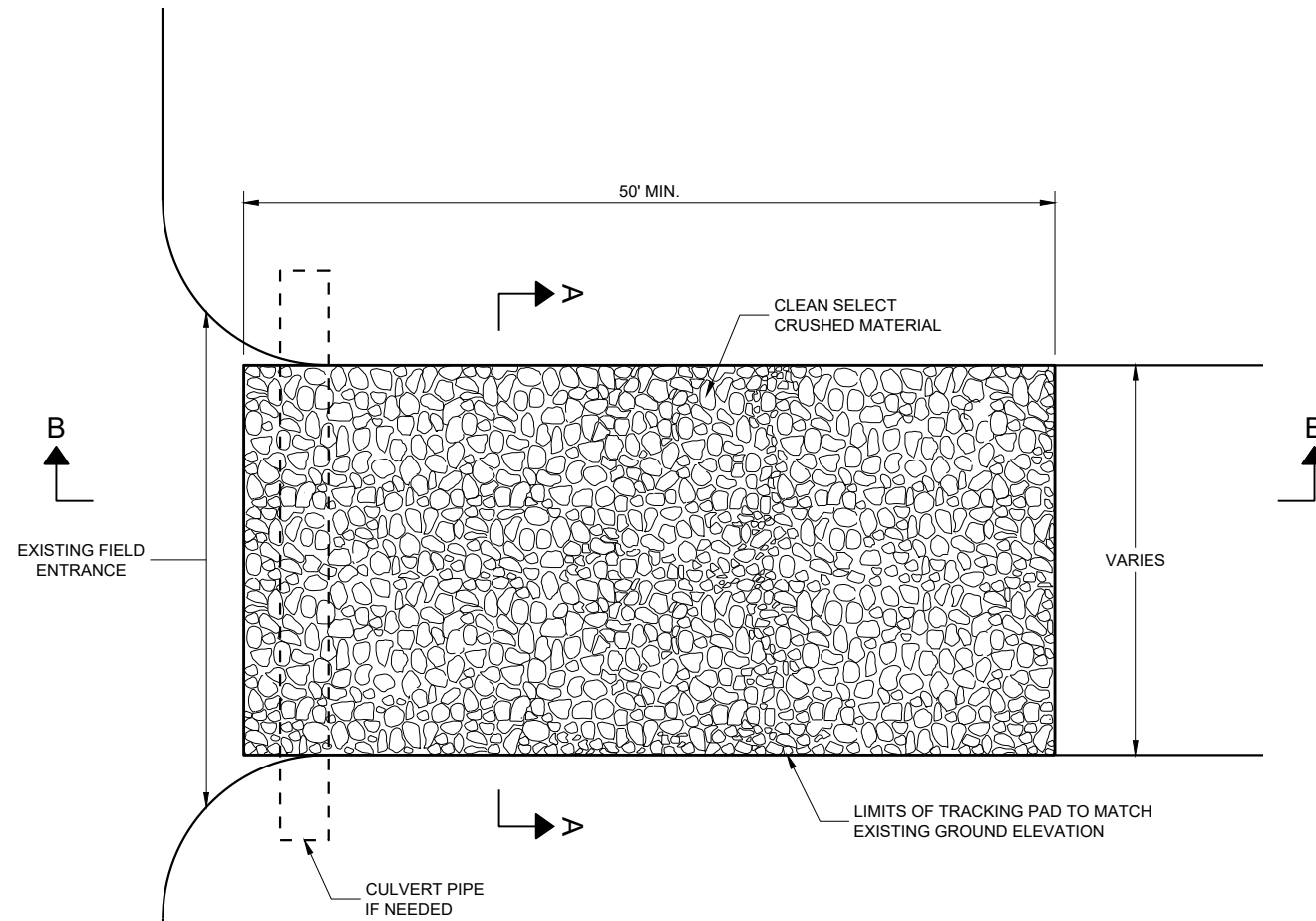
SURFACE WATER MUST BE PREVENTED FROM PASSING THROUGH THE TRACKING PAD. FLOWS SHALL BE DIVERTED AWAY, AROUND OR CONVEYED UNDER THE TRACKING PAD.

CULVERT PIPE OR OTHER BMP USED TO DIVERT WATER AWAY, AROUND OR UNDER THE TRACKING PAD SHALL BE DESIGNED TO CONVEY THE 2 YEAR - 24 HOUR EVENT.

THE COST OF ADDITIONAL BMP TO DIVERT WATER ARE INCIDENTAL TO THE TRACKING PAD BID ITEM.



SECTION A - A



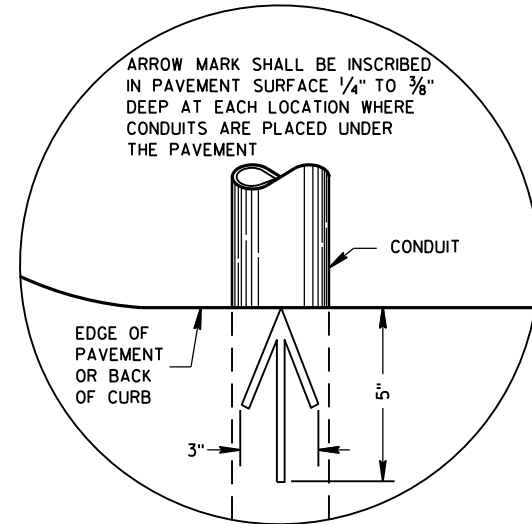
SECTION B - B

TRACKING PAD

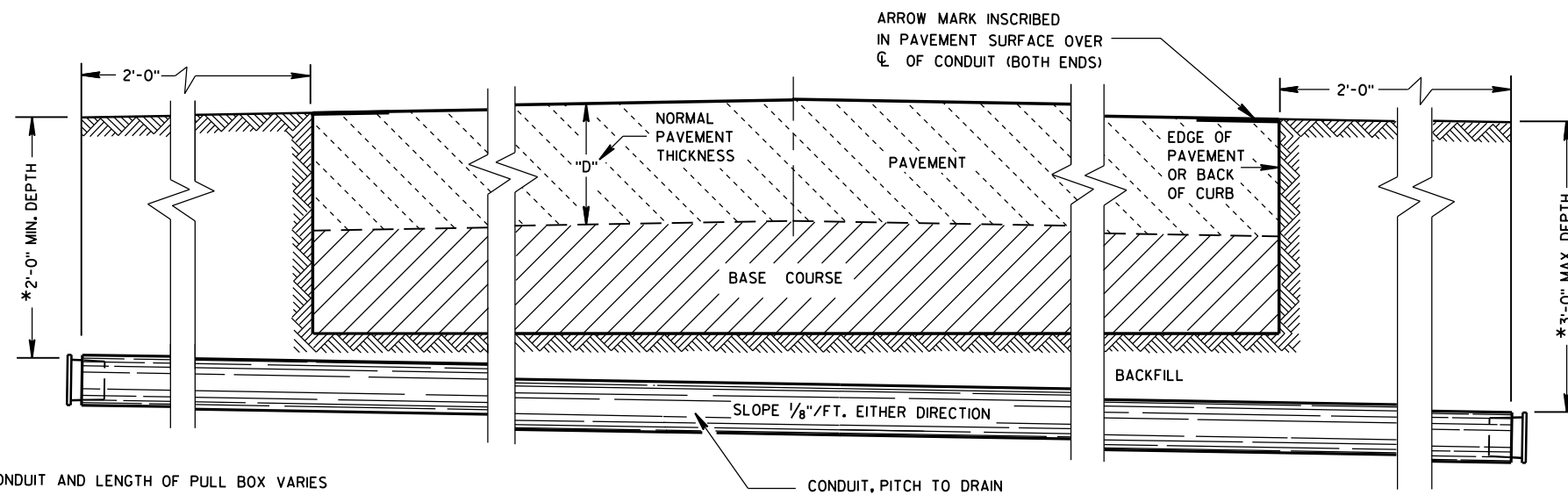
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
3/24/2011 DATE /S/ Jerry H. Zogg
ROADWAY STANDARDS DEVELOPMENT ENGINEER

FHWA



**PLAN VIEW
ARROW MARK**



**SIDE ELEVATION
DETAIL FOR CONDUIT UNDER PAVED HIGHWAYS**

*DEPTH OF CONDUIT AND LENGTH OF PULL BOX VARIES WITH HEIGHT OF CURB USED. ALSO SEE PULL BOX S.D.D. 9B4

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

METALLIC (STANDARD SPECIFICATION 652.2.2) OR NONMETALLIC (STANDARD SPECIFICATION 652.2.3) CONDUIT SHALL BE FURNISHED AND PLACED AS SHOWN.

DEPTH OF CONDUIT INSTALLED BELOW THE TRAVELED WAY SHALL BE 24 INCHES MINIMUM AND 36 INCHES MAXIMUM.

DEPTH OF CONDUIT INSTALLED THAT IS NOT BELOW THE TRAVELED WAY SHALL BE 18 INCHES MINIMUM AND 36 INCHES MAXIMUM.

ANY EXCEPTION TO THE MAXIMUM DEPTH SHALL BE ONLY WITH THE WRITTEN APPROVAL OF THE ENGINEER.

THE TRENCH SHALL NOT BE BACKFILLED PRIOR TO INSPECTION OF THE CONDUIT.

ALL METALLIC CONDUIT RACEWAY ENDS SHALL BE REAMED AND THREADED.

ALL METALLIC CONDUIT IN WHICH WIRE OR CABLE IS TO BE INSTALLED SHALL BE BUSHED WITH APPROVED THREADED BUSHINGS BEFORE INSTALLATION OF THE WIRE OR CABLE.

ALL METALLIC CONDUITS IN WHICH WIRE OR CABLE IS NOT TO BE INSTALLED SHALL BE CAPPED WITH THREADED PROTECTIVE CAPS, AS APPROVED BY THE ENGINEER.

ALL NONMETALLIC CONDUIT SHALL BE CAPPED OR PLUGGED IMMEDIATELY AFTER INSTALLATION AND SHALL REMAIN CAPPED OR PLUGGED UNTIL WIRE/CABLES ARE INSTALLED.

NONMETALLIC CONDUITS IN WHICH WIRE OR CABLE IS NOT BEING INSTALLED SHALL REMAIN CAPPED OR PLUGGED.

BENDING OF PVC ELECTRICAL CONDUIT SHALL BE ACCOMPLISHED BY USING A BLANKET OR EMERSION TYPE TANK DESIGNED FOR THE PURPOSE OF BENDING PVC ELECTRICAL CONDUIT.

ALL CUT ENDS SHALL BE TRIMMED INSIDE AND OUTSIDE TO REMOVE ALL ROUGH EDGES ON NONMETALLIC CONDUIT. (SEE NEC 347.5)

WHEN REQUIRED TO CONNECT NONMETALLIC CONDUIT TO METALLIC CONDUIT, ONLY U.L. LISTED ADAPTER FITTINGS SHALL BE USED.

PRIOR TO CONDUIT ACCEPTANCE, CONDUIT CAPS OR PLUGS SHALL BE REMOVED, AND THE CAPS, PLUGS AND CONDUIT ENDS SHALL BE THOROUGHLY CLEANED AND THEN THE CAPS OR PLUGS REINSTALLED TO ENSURE THAT THE CAPS OR PLUGS CAN BE EASILY REMOVED IN THE FUTURE.

ALL CONDUIT BEING FURNISHED AND INSTALLED SHALL HAVE THE U.L. LABEL FIRMLY ATTACHED.

CONDUIT RUNS SHALL BE THE SAME SIZE OF CONDUIT FROM ONE END TO THE OTHER (FROM PULL BOX TO PULL BOX-OR-JUNCTION BOX TO JUNCTION BOX-OR-BASE TO BASE, ETC.).

TRACER WIRE SHALL BE INSTALLED AS STATED IN THE STANDARD SPECIFICATION, ITEM 652.3.1.1.

ALL CONDUIT RUNS SHALL BE STRAIGHT (WITHOUT BENDS) FROM PULL BOX TO PULL BOX, PULL BOX TO BASE AND BASE TO BASE AS SHOWN ON THE PLANS.

6

6

S.D.D. 9 B 2-10

S.D.D. 9 B 2-10

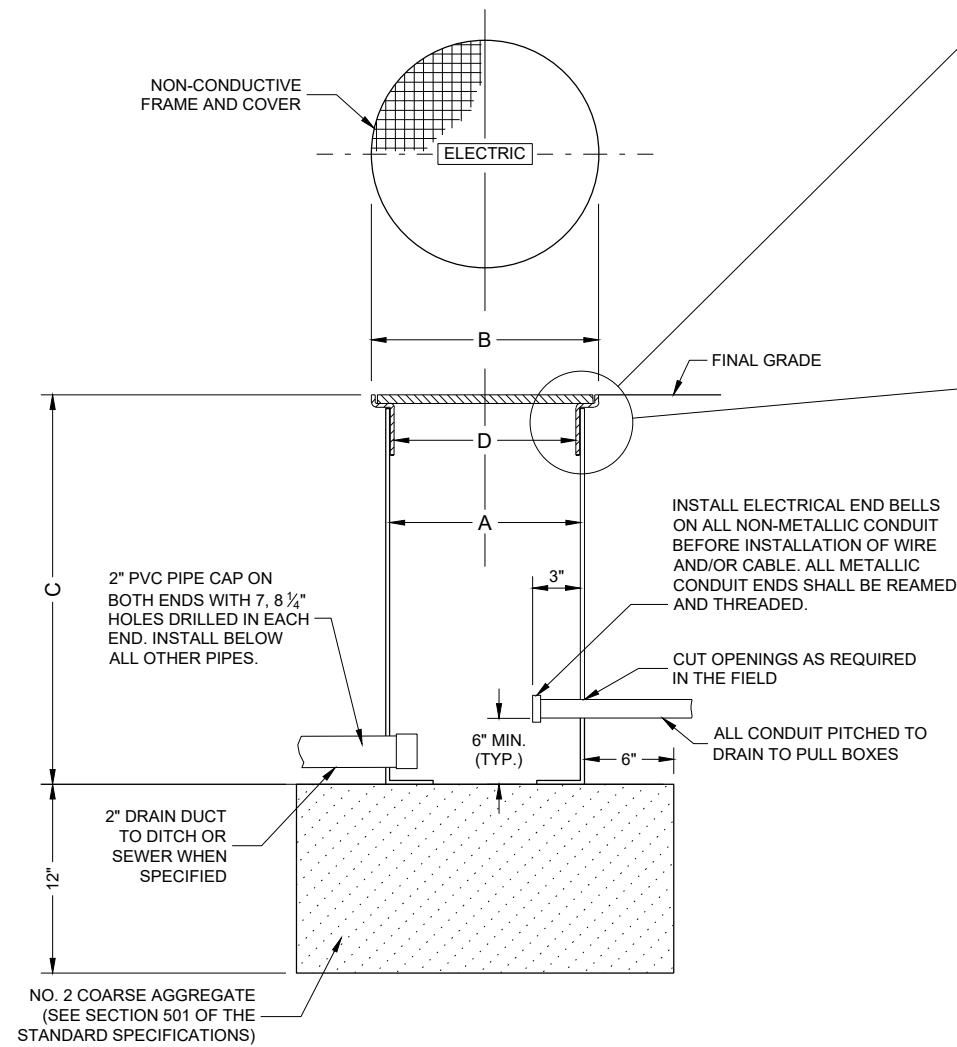
CONDUIT	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED March, 2017 DATE	/S/ Ahmet Demirbilek STATE ELECTRICAL ENGINEER
FHWA	

TABLE OF NOMINAL DIMENSIONS AND WEIGHTS

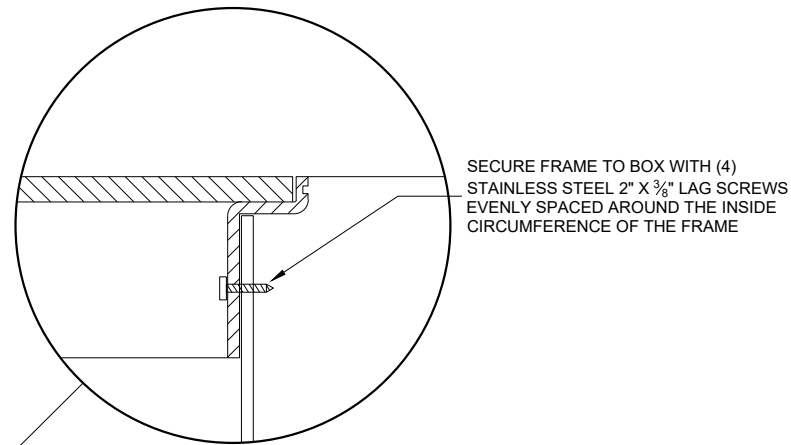
DIMENSION IN INCHES		NON- CONDUCTIVE PULL BOX	
BOX DIAMETER ** (INSIDE)	A	24	24
BOX OVERALL OUTSIDE DIAMETER	B	27	27
BOX LENGTH	C	36	42
FRAME OPENING	D	22 ½	22 ½
WEIGHT IN POUNDS *			
COVER		50	50
BOX ONLY		75	85

* THE ACTUAL WEIGHT OF THE COVER OR BOX ONLY MAY VARY NOT TO EXCEED 100 LBS INDIVIDUALLY.

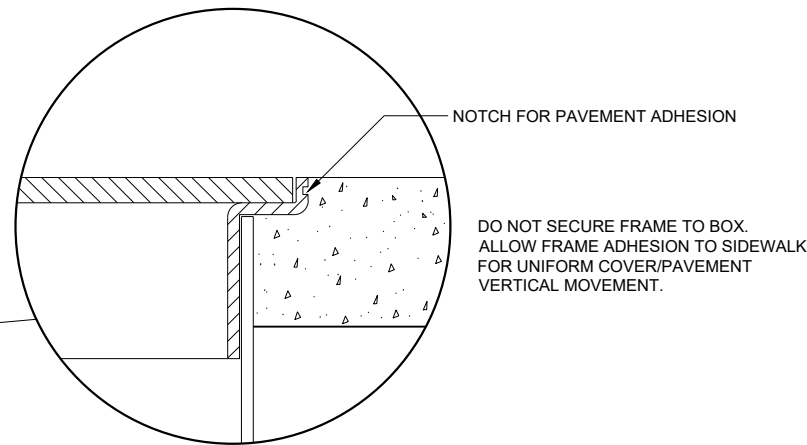
** DIAMETER VARIES FROM TOP TO BOTTOM WITH THE DIAMETER LARGER AT THE BOTTOM TO PREVENT FROST HEAVE.



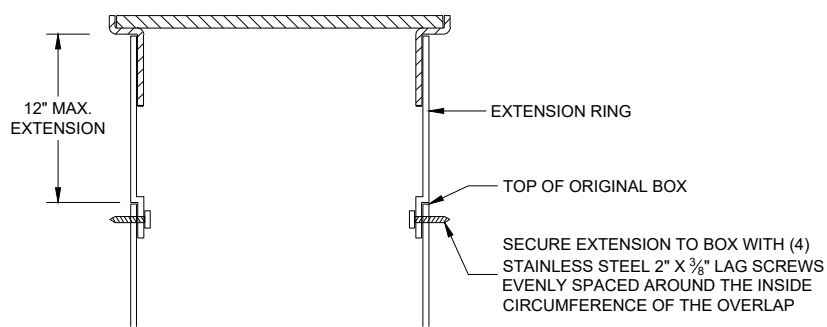
NON-CONDUCTIVE PULL BOX



INSTALLED IN SOD OR CRUSHED AGGREGATE



INSTALLED IN SIDEWALK



BOX EXTENSION

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

ALL BOXES, FRAMES AND COVERS SHALL BE SUITABLE FOR TIER 15 LOADING AS SPECIFIED IN ANSI/SCTE 77.

PROVIDE AN OPENING FOR TOOL ASSISTED COVER REMOVAL NOT LARGE ENOUGH TO PERMIT PASSAGE OF A SPHERE MORE THAN ½" DIAMETER

ENSURE COVER SURFACE IS SKID RESISTANT WITH A COEFFICIENT OF FRICTION OF AT LEAST 0.5 AND VERTICAL SURFACE DISCONTINUITIES LESS THAN ¼".

COVER SHALL BE MAGNETICALLY LOCATABLE.

BOXES AND EXTENSIONS ARE TRIMMABLE FOR CUSTOM LENGTHS. TRIMMED PIECES SHALL MAINTAIN A UNIFORM LENGTH.

ENTRANCE HOLES INTO PULL BOXES SHALL BE CUT WITH A CIRCULAR HOLE SAW OR HYDRAULIC CONDUIT PUNCH. HOLE SIZE SHALL BE THE OUTSIDE DIAMETER OF THE CONDUIT THAT IS TO FIT IN THE OPENING PLUS NO MORE THAN ¼".

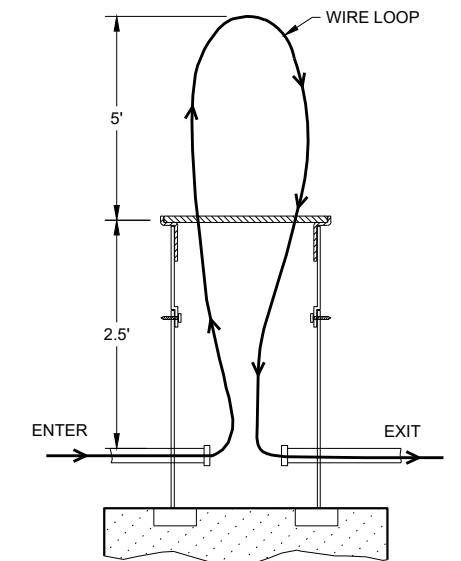
THE CONTRACTOR SHALL NOT INSTALL WIRE IN ANY PULL BOX UNTIL ITS INSTALLATION HAS BEEN INSPECTED AND ACCEPTED BY THE ENGINEER.

ALL METALLIC CONDUIT IN WHICH WIRE AND/OR CABLE IS TO BE INSTALLED, SHALL BE BUSHED BEFORE INSTALLATION OF THE WIRE AND/OR CABLE.

ENTIRE BOX MUST BE CONSTRUCTED OF NON-CONDUCTIVE MATERIALS WITH THE EXCEPTION OF STAINLESS STEEL FASTENERS AND MAGNETIC LOCATABLE DEVICE.

WHEN A PULL BOX IS INSTALLED IN CRUSHED AGGREGATE SHOULDERS, PLACE IT 2-3 INCHES BELOW GRADE AND COVER IT WITH 2-3 INCHES OF CRUSHED AGGREGATE.

LABEL ON COVER SHALL READ "ELECTRIC" FOR SIGNAL AND LIGHTING SYSTEMS, "WISDOT ITS" FOR COMMUNICATIONS AND ITS EQUIPMENT SYSTEMS.



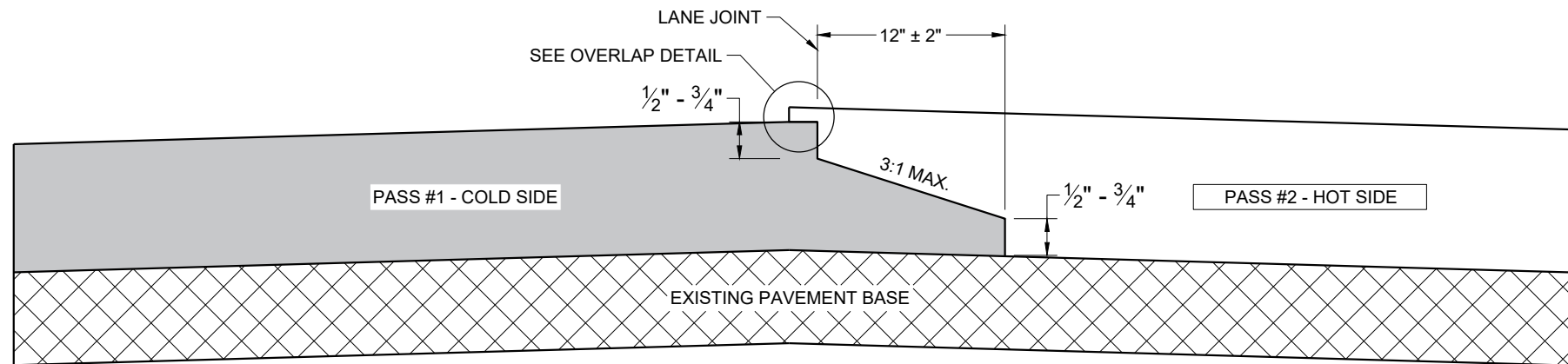
MEASUREMENT DETAIL FOR WIRE/CABLE IN THE PULL BOX

PULL BOXES NON-CONDUCTIVE

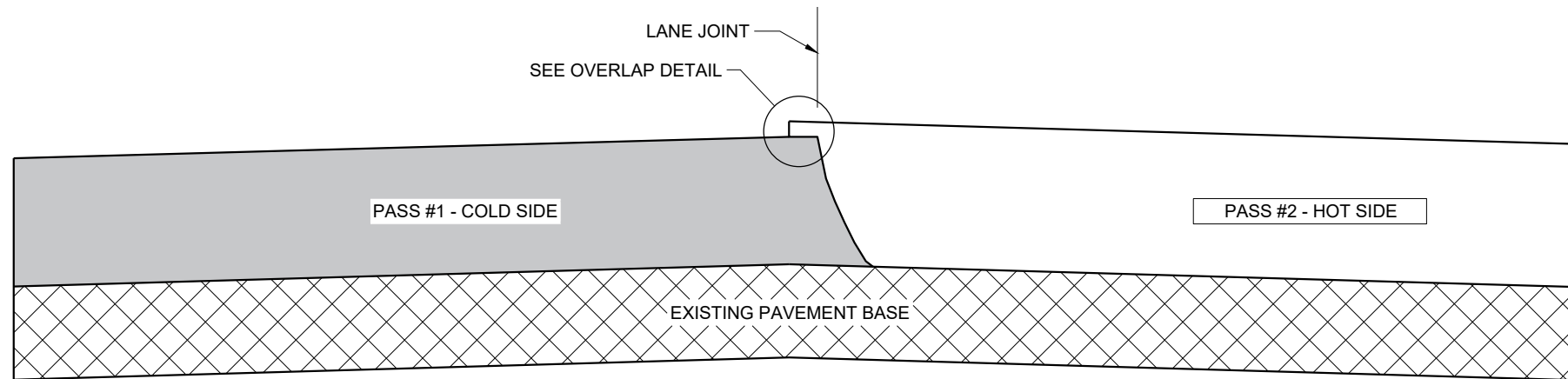
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

APPROVED
 May 2022 /S/ Ahmet Demirelek
 DATE STATE ELECTRICAL ENGINEER

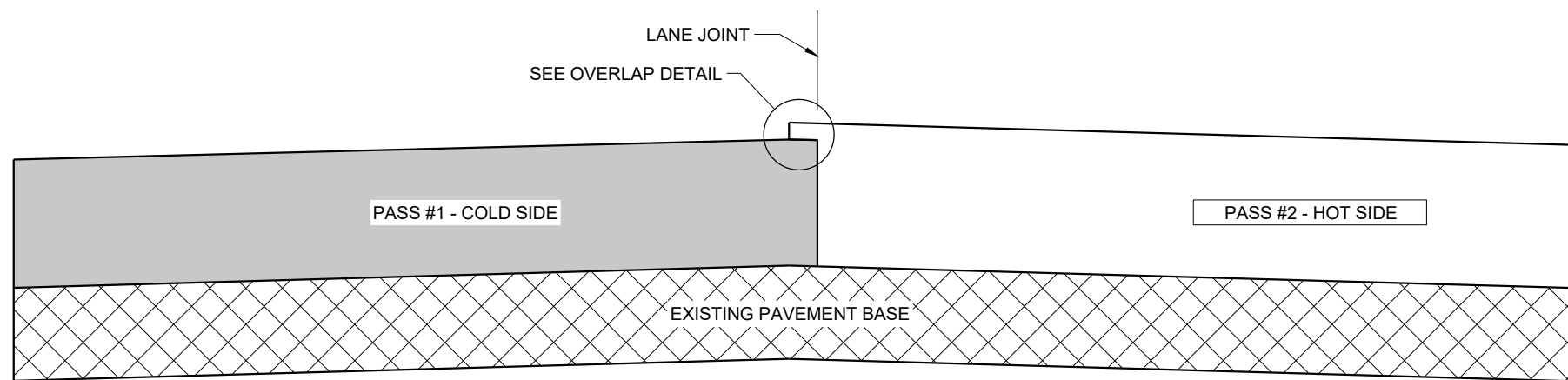
FHWA



**TYPICAL PAVEMENT CROSS SECTION
NOTCHED WEDGE JOINT**



**TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT**



**TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT (MILLED)**

GENERAL NOTES

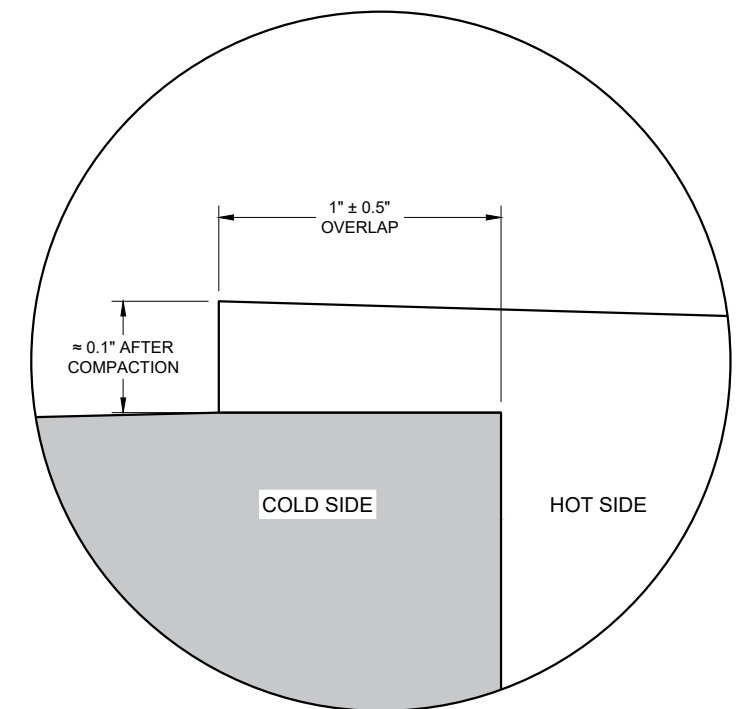
IN ADDITION TO THE DETAILS PROVIDED IN THIS DRAWING, CONFORM TO STANDARD SPECIFICATION 450.3.2.8 FOR WHEN A NOTCHED WEDGE JOINT IS REQUIRED AND FOR GENERAL JOINT CONSTRUCTION REQUIREMENTS.

FOR ALL LONGITUDINAL JOINTS, ENSURE THE PAVER SCREED OVERLAPS THE PREVIOUSLY PLACED PAVEMENT BY $1" \pm 0.5"$ AND THE HOT SIDE OF THE JOINT REMAINS HIGHER THAN THE COLD SIDE BY APPROXIMATELY 0.1" AFTER FINAL COMPACTION. (IT WILL BE FLUSH WHEN PAVING IN ECHELON.)

ONLY REMOVE THE LONGITUDINAL NOTCHED WEDGE JOINT FOR SMA PAVEMENT OR AS DIRECTED BY THE ENGINEER TO ADDRESS SPECIFIC LENGTHS OF JOINT DAMAGED BY TRAFFIC.

WHEN MILLING BACK OR REMOVING ANY LONGITUDINAL JOINT, LIMIT THE MATERIAL REMOVED TO 2" FROM THE TOP NOTCH OR FROM THE VERTICAL JOINT EDGE ON THE COLD SIDE OF THE JOINT.

USE LONGITUDINAL MILLED JOINT AS PLANS SHOW OR THE AS THE ENGINEER DIRECTS.



OVERLAP DETAIL (TYPICAL)

6

6

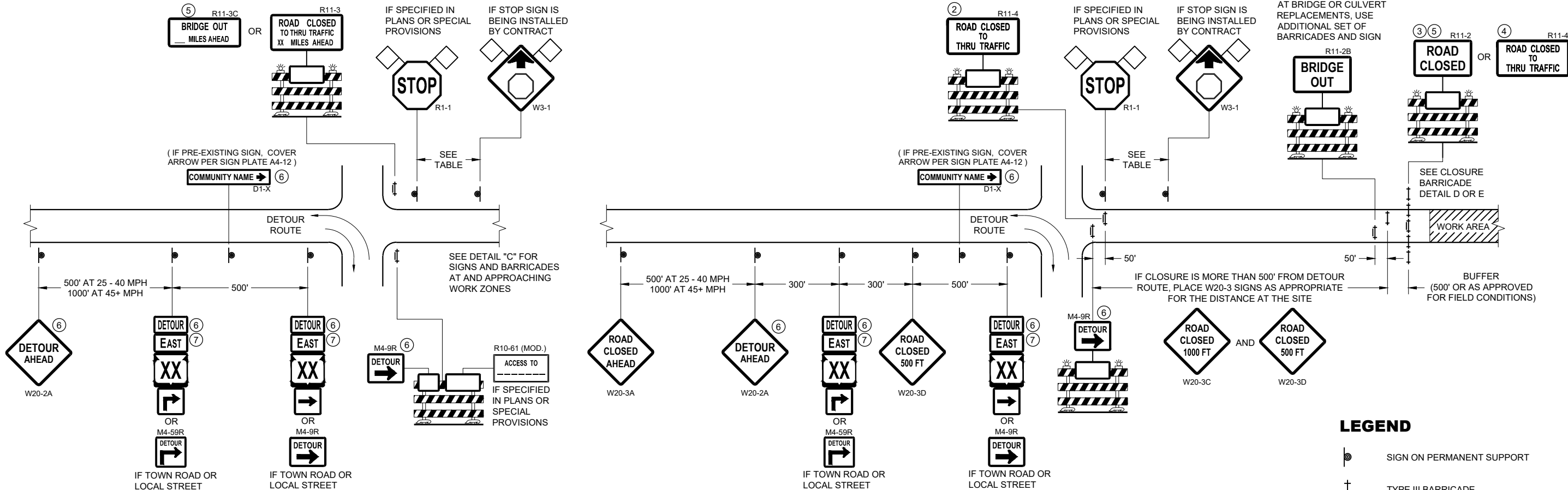
SDD 13C19 - 03

SDD 13C19 - 03

HMA LONGITUDINAL JOINTS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2020 /S/ Steven Hefel
DATE HMA PAVEMENT ENGINEER
FHWA



**DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR**

WORK ZONE GREATER THAN OR EQUAL TO 1/2 MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)

**DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR**

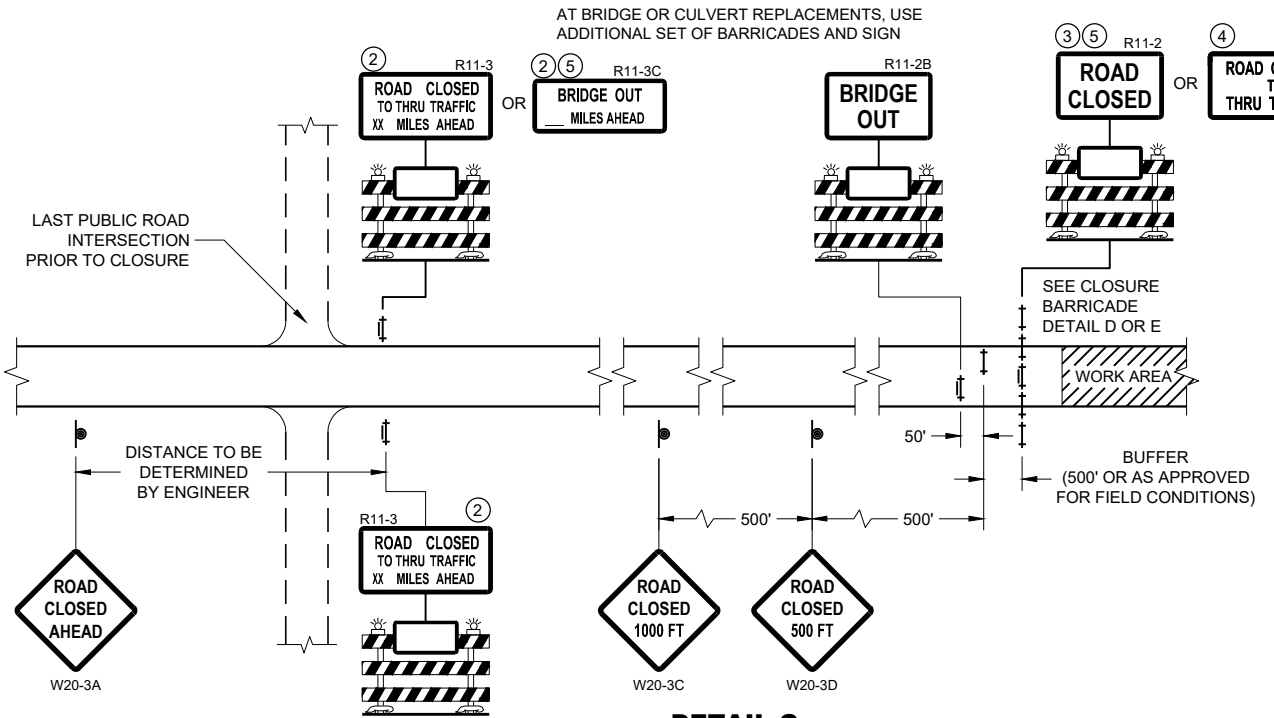
WORK ZONE LESS THAN 1/2 MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
- FLAGS, 16" X 16" MIN. (ORANGE)

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

- M4 - 8
- M3 - X
- OR OR M1 - 4 M1 - 6 M1 - 5A
- OR M05 - 1 M06 - 1



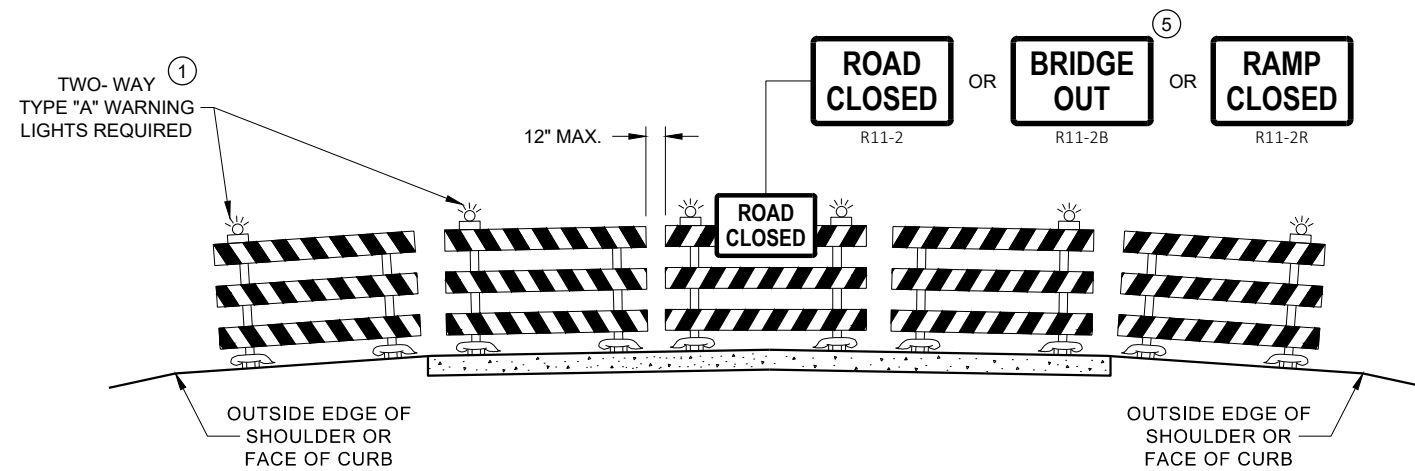
**DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR**

SEE SDD 15C2-SHEET "b"
FOR GENERAL NOTES
AND FOOTNOTES ① THROUGH ⑦

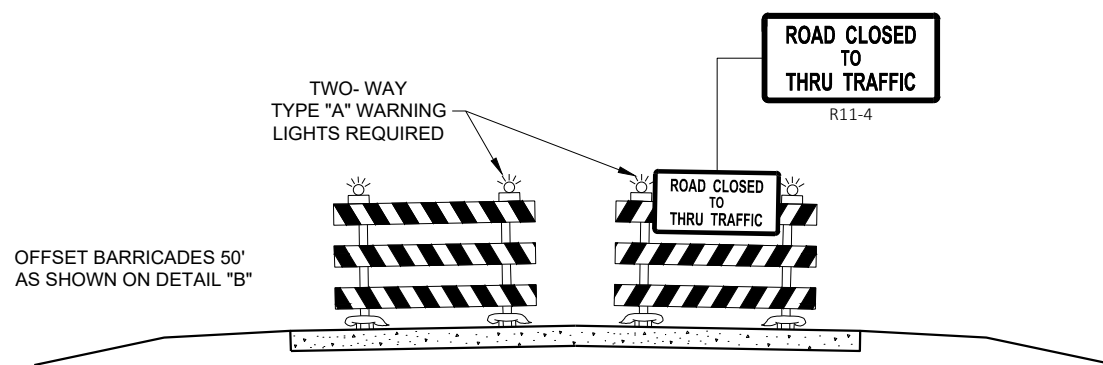
**BARRICADES AND SIGNS
FOR MAINLINE CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 /S/ Andrew Heidtke
DATE DATE WORK ZONE ENGINEER
FHWA



**DETAIL D
ROAD CLOSURE BARRICADE DETAIL
APPROACH VIEW**



**DETAIL E
LANE CLOSURE BARRICADE DETAIL
APPROACH VIEW**

SEE SDD 15C2 - SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW - INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11 - 2, R11 - 3, M4 - 9, R11 - 4, AND R10 - 61 SIGNS PLACED ON THE BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE RAIL OR BOTTOM RAILS.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

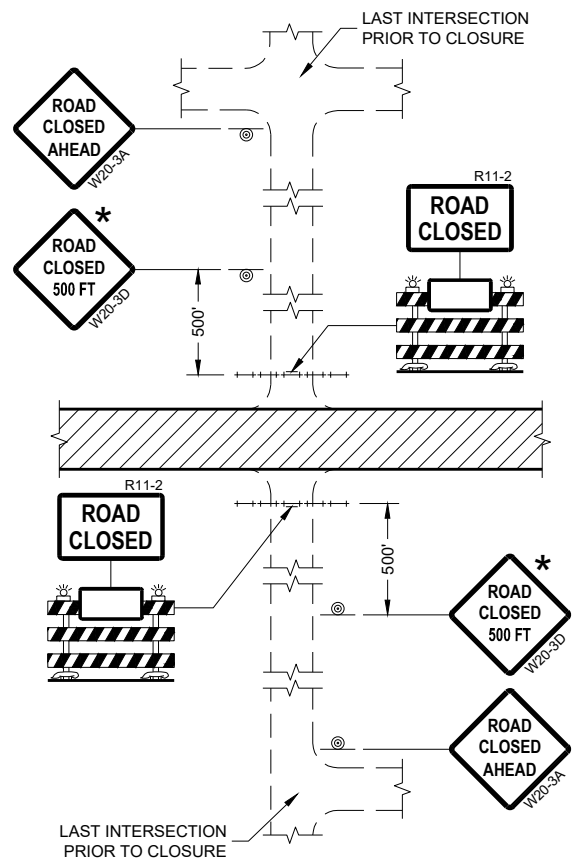
- R11 - 2 SHALL BE 48" X 30"
- R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60" X 30"
- M4 - 9 SHALL BE 30" X 24"
- M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1 - 1 SHALL BE 36" X 36"

- ① TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT SPACING).
- ② THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.
- ③ FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".
- ④ FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".
- ⑤ FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.
- ⑥ INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.
- ⑦ "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

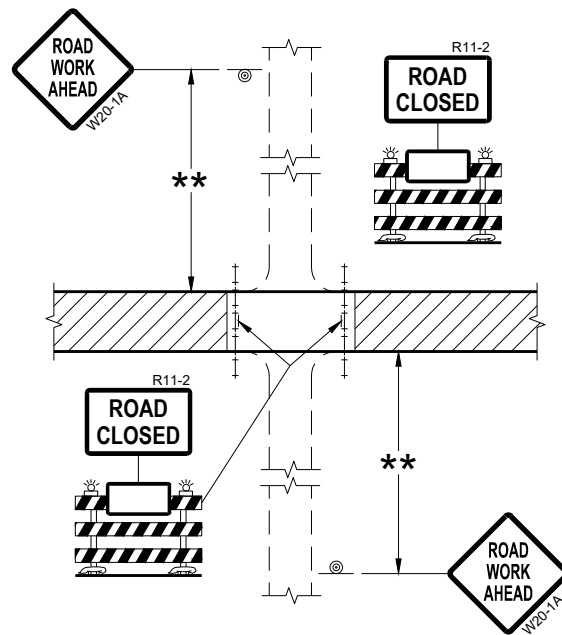
**BARRICADES AND SIGNS
FOR
VARIOUS CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

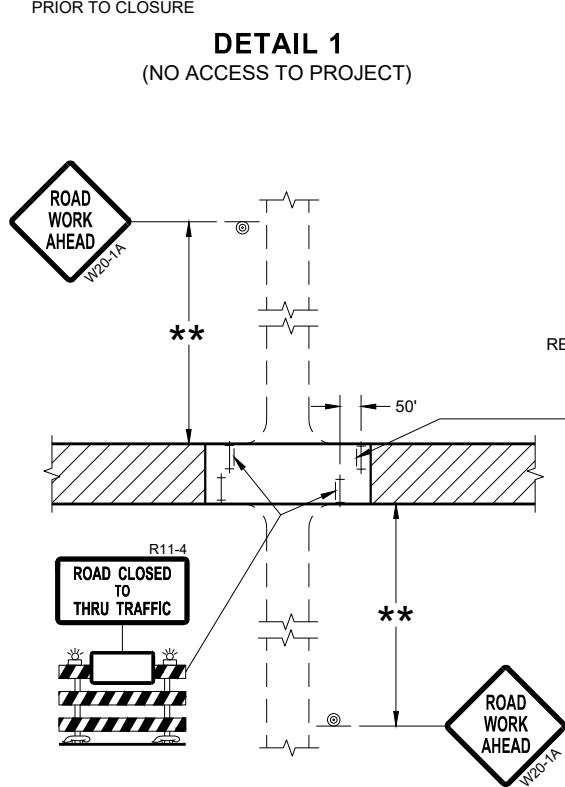
APPROVED
May 2023 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER



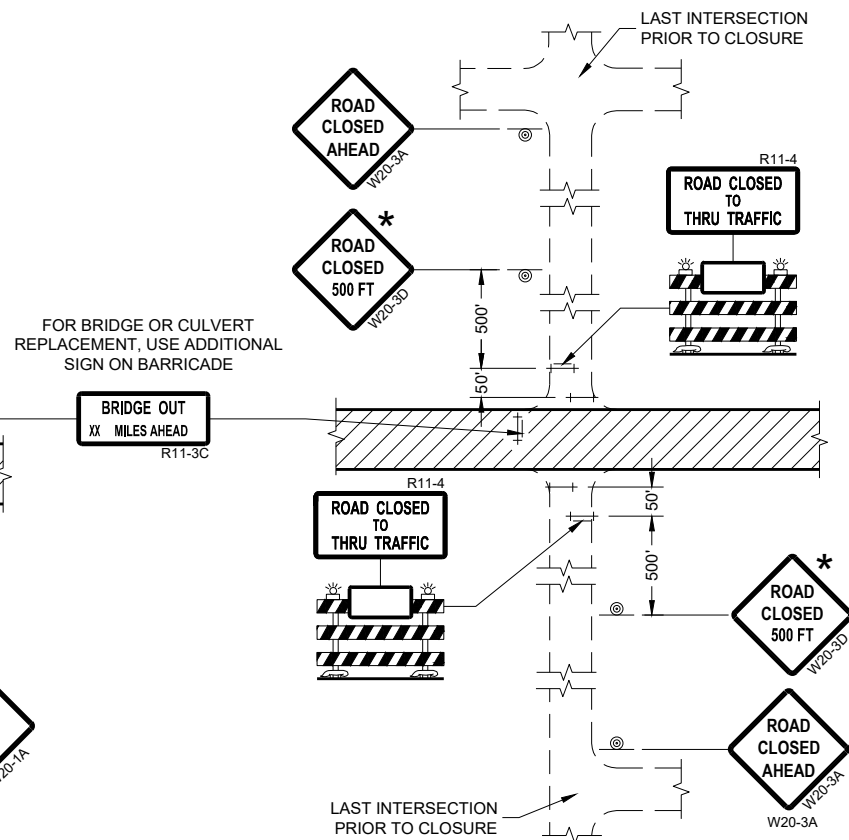
DETAIL 1
(NO ACCESS TO PROJECT)



DETAIL 2
(PUBLIC CROSS-TRAFFIC MAINTAINED.
NO ACCESS TO PROJECT)



DETAIL 3
(PUBLIC CROSS-TRAFFIC MAINTAINED.
CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)



DETAIL 4
(CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE (500 FEET DESIRABLE) TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3, AND R11-4 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
R11-2 SHALL BE 48" X 30".
R11-4 AND R11-3 SHALL BE 60" X 30".

- * OMIT THE "ROAD CLOSED 500 FT." SIGN IF THE LAST INTERSECTION IS 500 FEET OR LESS FROM THE WORK ZONE.
- ** 500' MAX. OR AT LAST INTERSECTION, WHICHEVER IS CLOSEST.

LEGEND

- ⊙ SIGN ON PERMANENT SUPPORT
- ⊥ TYPE III BARRICADE
- ⊥ TYPE III BARRICADE WITH ATTACHED SIGN
- ⚡ TYPE "A" WARNING LIGHT (FLASHING)
- ▨ WORK AREA

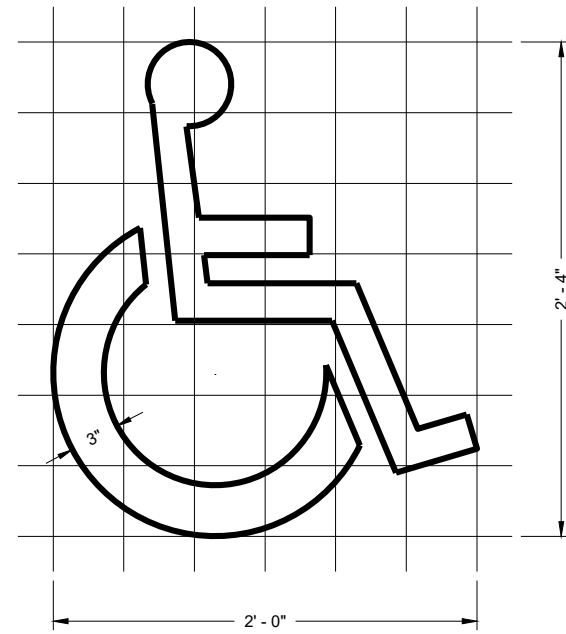
**BARRICADES AND SIGNS
FOR
SIDEROAD CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

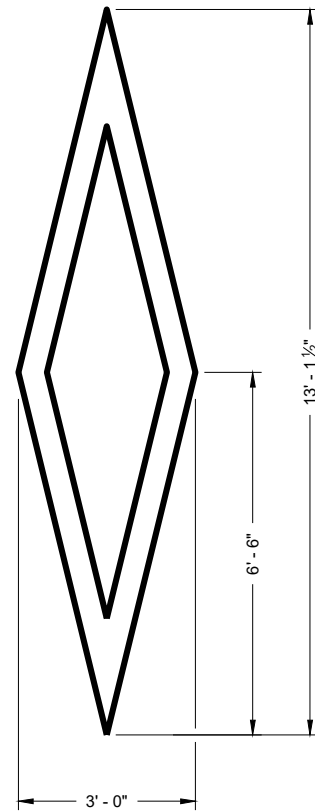
APPROVED
July 2018 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

GENERAL NOTES

DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.



HANDICAP SYMBOL

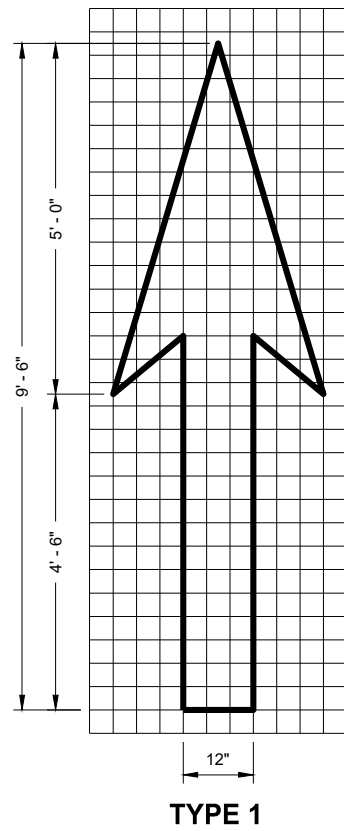


PREFERENTIAL LANE SYMBOL

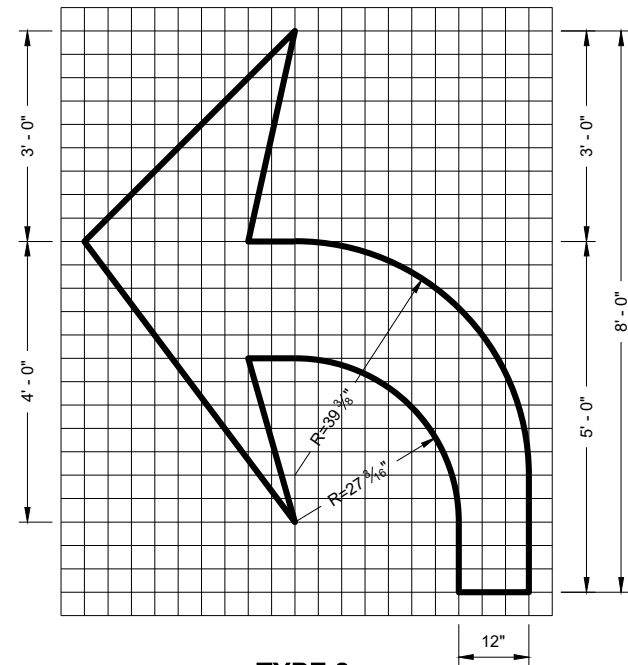
PAVEMENT MARKING SYMBOLS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

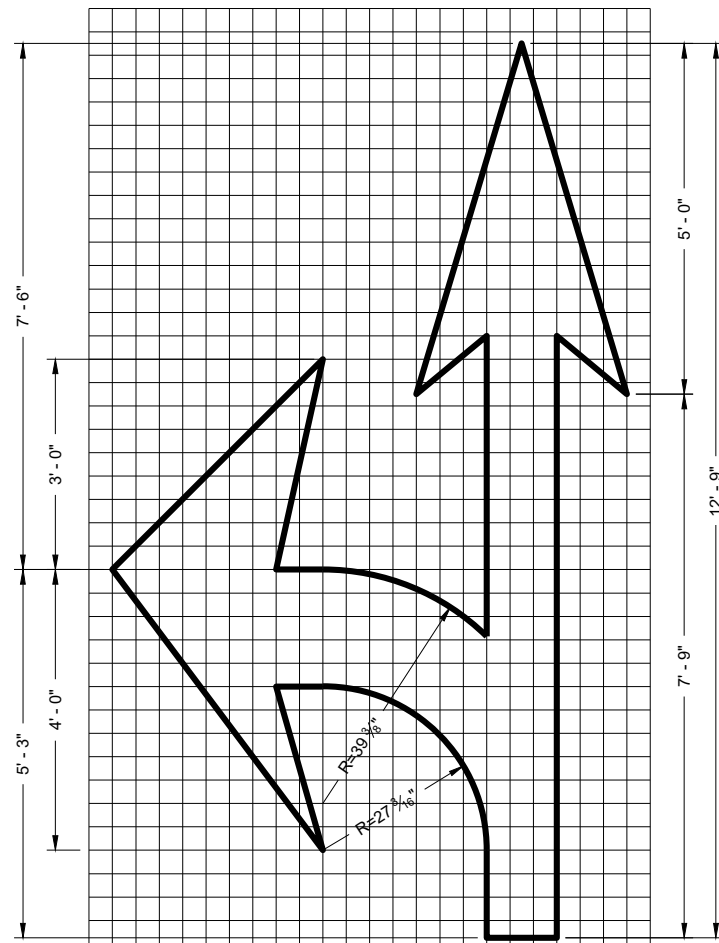
APPROVED
November 2019 /S/ Matthew Rauch
DATE STATE SIGNING AND MARKING
ENGINEER



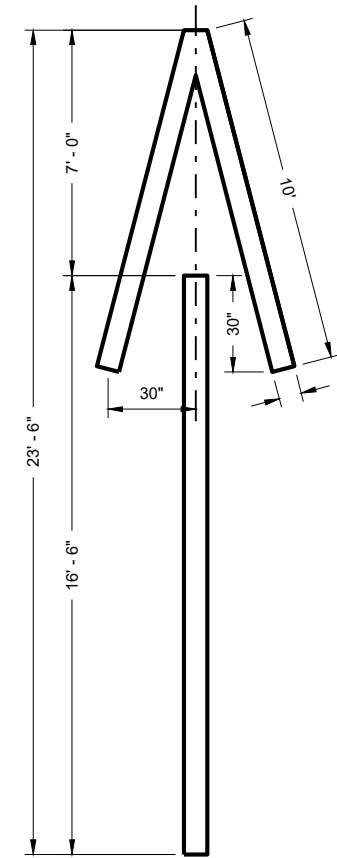
TYPE 1



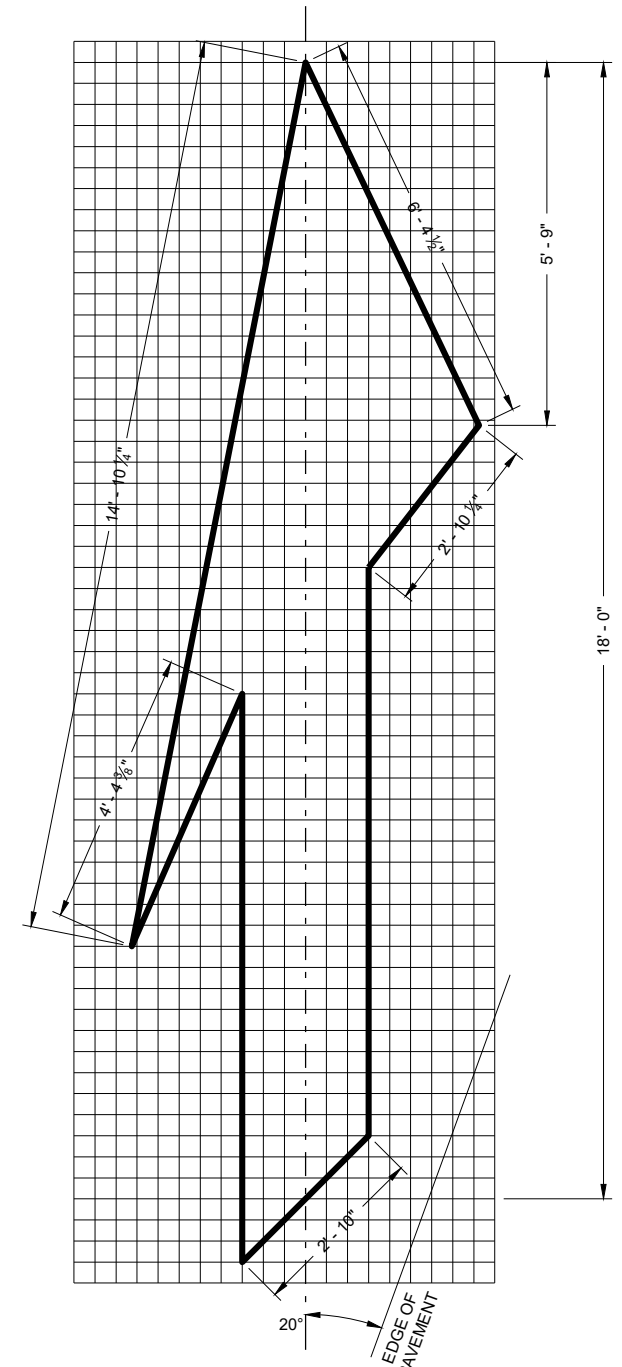
TYPE 2



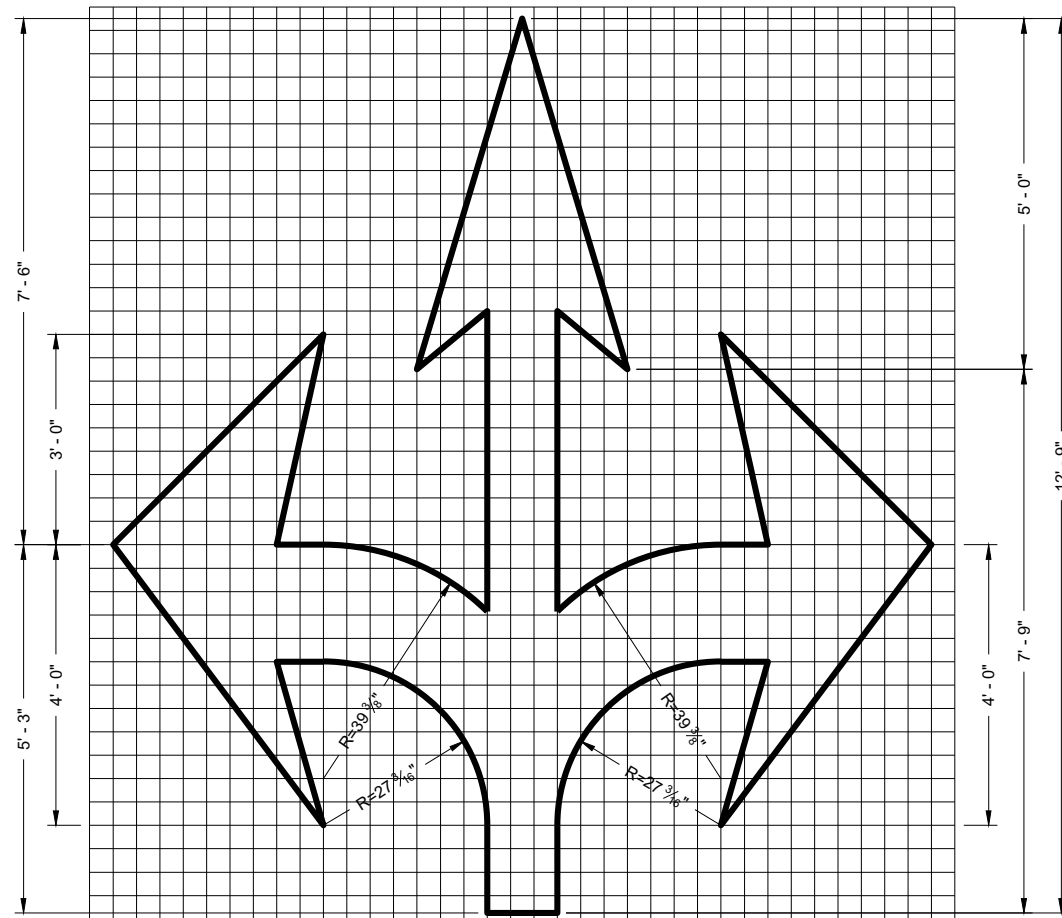
TYPE 3



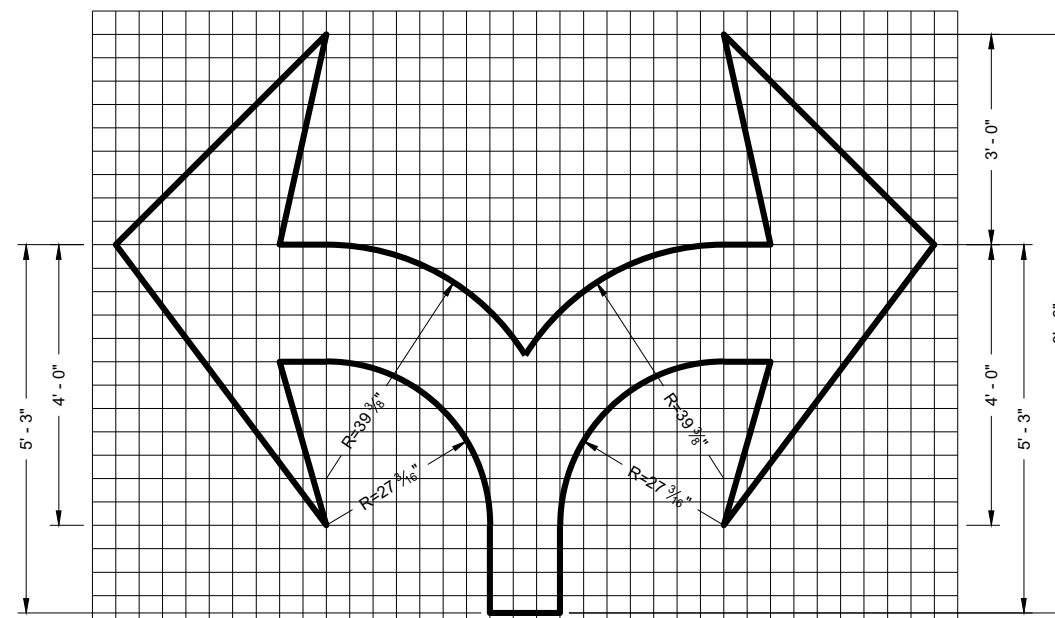
TYPE 4



TYPE 5 LANE DROP ARROW



TYPE 6



TYPE 7

GENERAL NOTES

DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

PAVEMENT MARKING ARROWS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED

November 2019

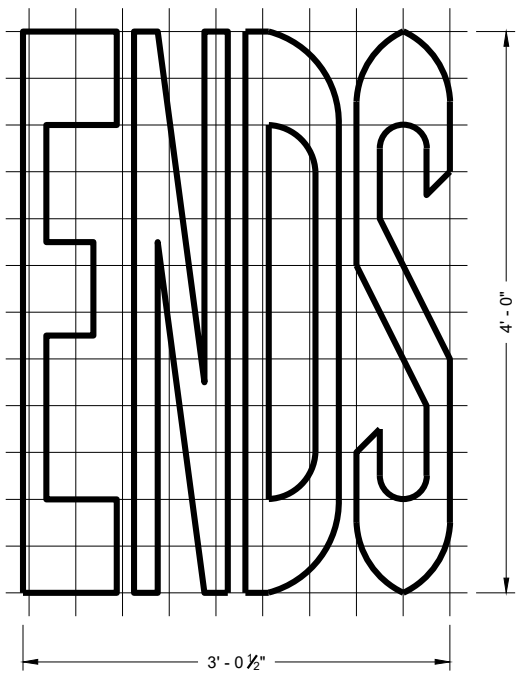
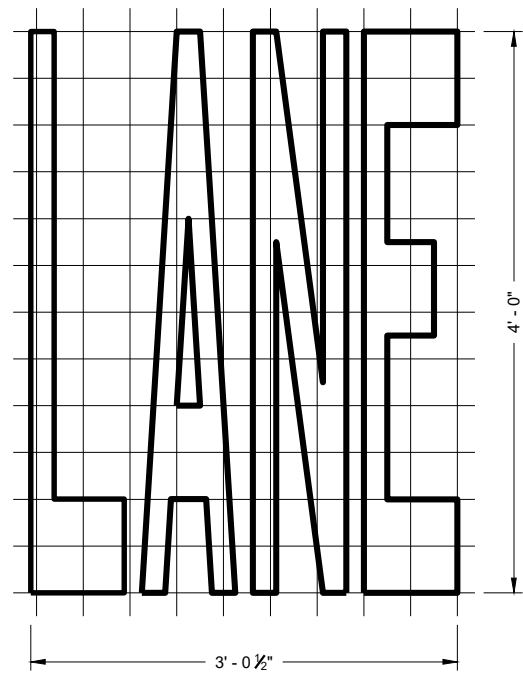
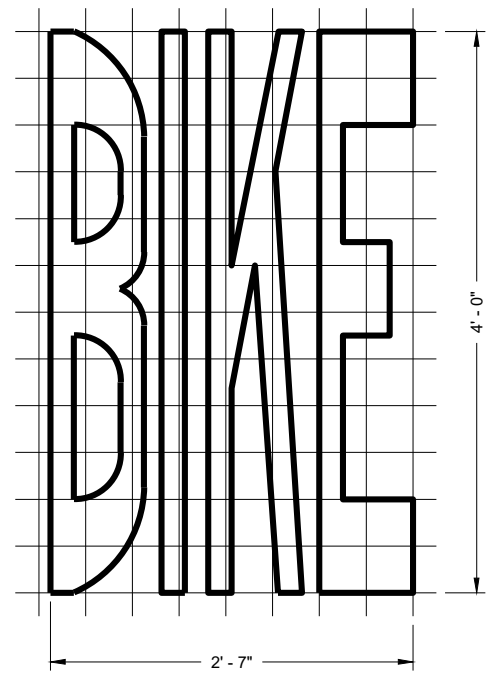
DATE

FHWA

/S/ Matthew Rauch
STATE SIGNING AND MARKING
ENGINEER



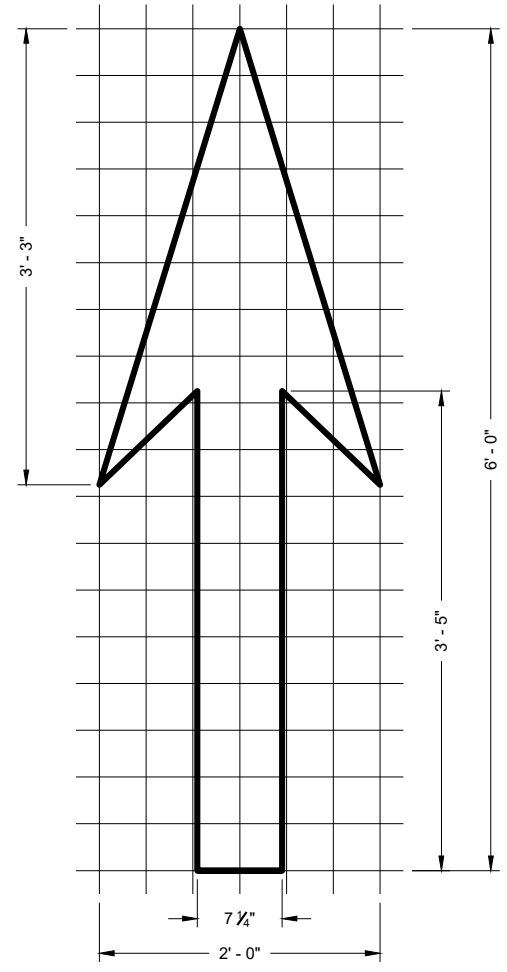
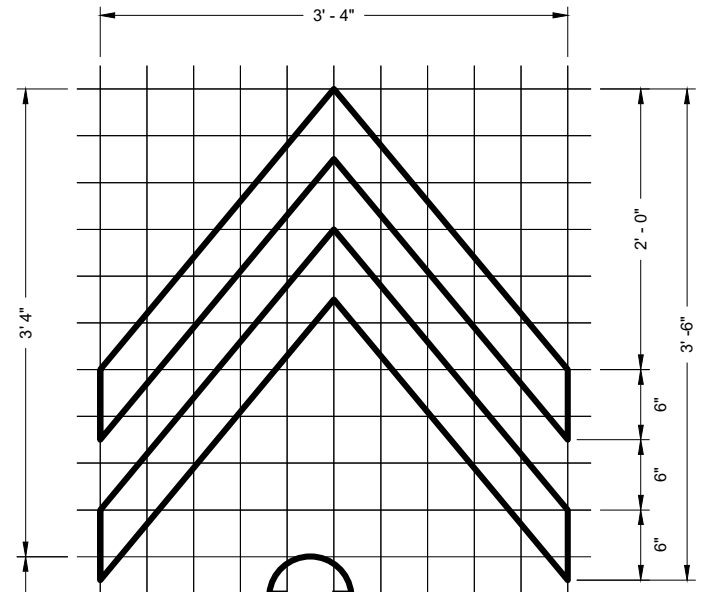
SDD 15C7-e Pavement Marking For Bike Lanes



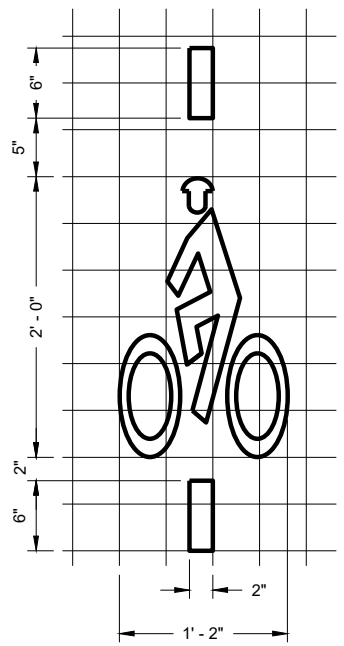
BIKE LANE WORDS

GENERAL NOTES

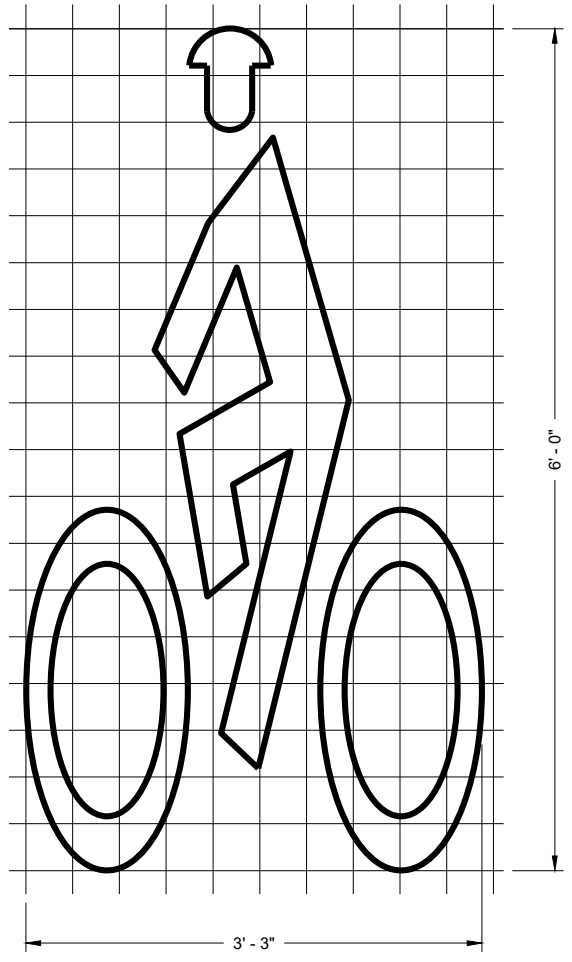
DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.



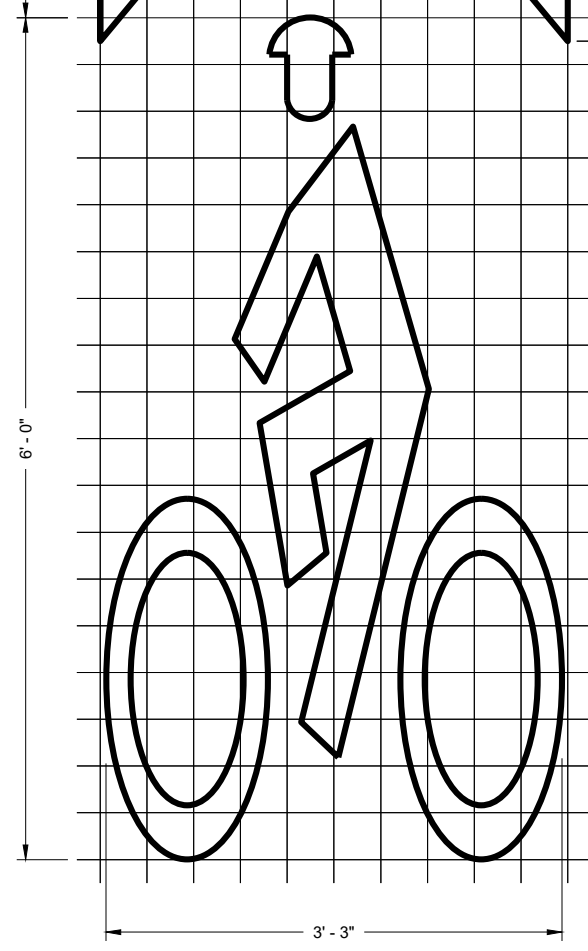
BIKE LANE ARROW



BICYCLE DETECTOR PAVEMENT MARKING



BIKE LANE SYMBOL



BIKE LANE SYMBOL FOR SHARED LANE

6

6

SDD 15C07 - 15e

SDD 15C07 - 15e

PAVEMENT MARKING FOR BIKE LANES	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED November 2019 DATE	/s/ Matthew Rauch STATE SIGNING AND MARKING ENGINEER
FHWA	

GENERAL NOTES

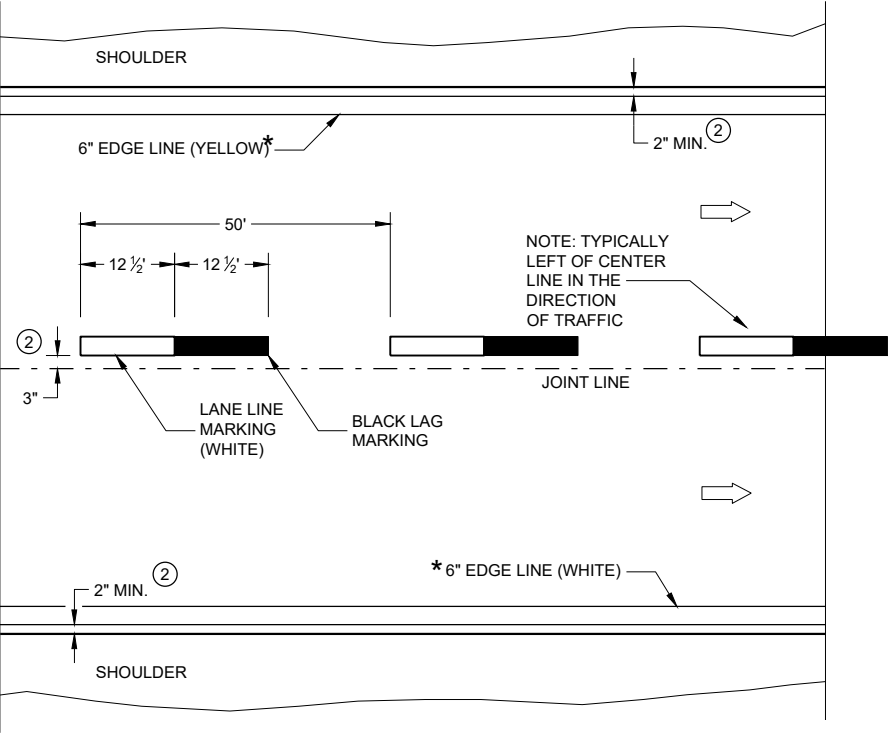
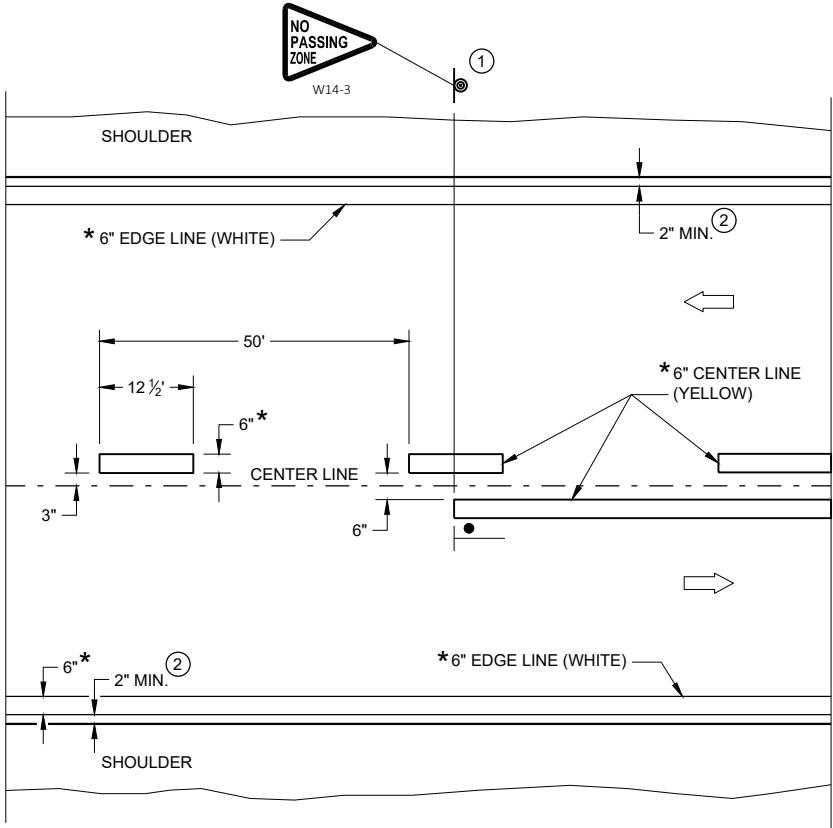
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

- ① LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING
- ② MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

LEGEND

- ⊥ "T" MARKING
- ⊙ SIGN ON PERMANENT SUPPORT
- ➔ DIRECTION OF TRAFFIC

* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES



TWO WAY TRAFFIC

ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING

6

6

SDD 15C08-23a

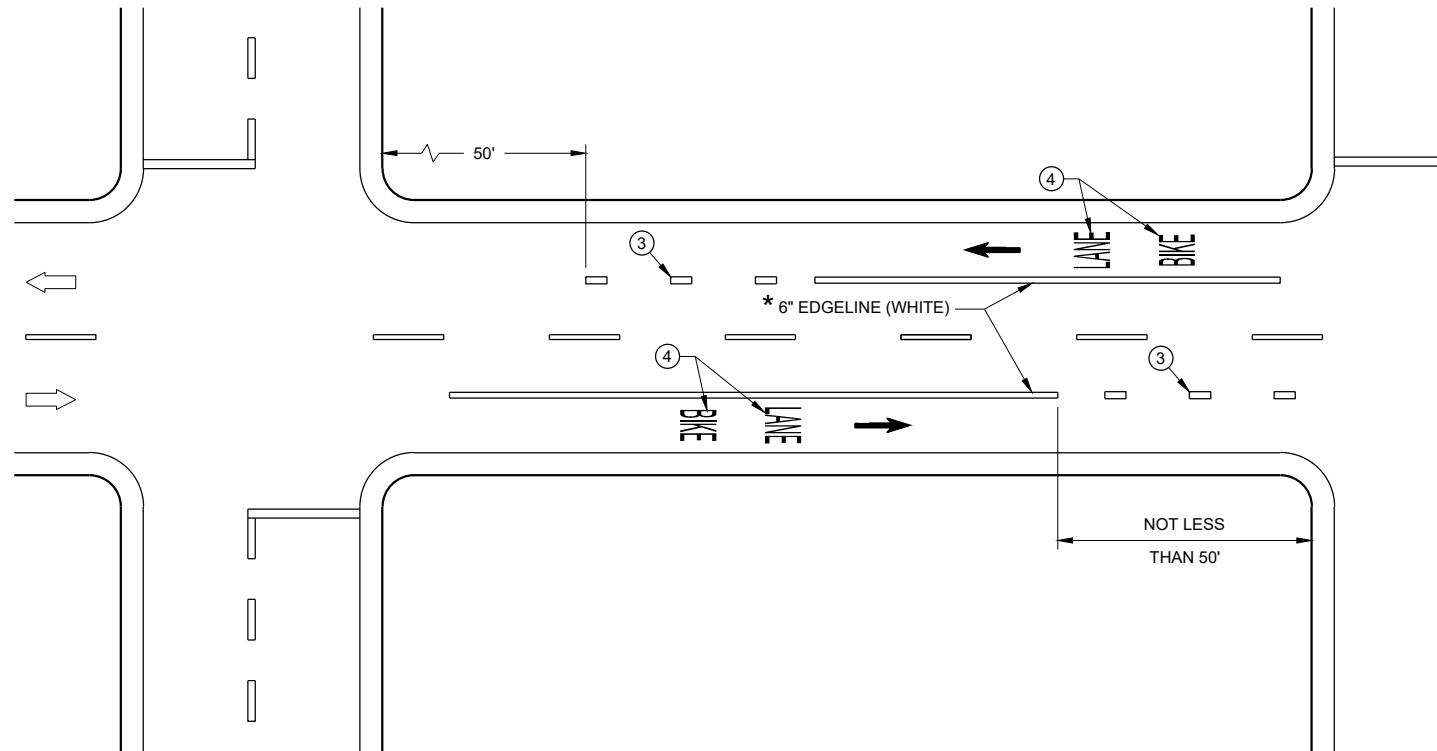
SDD 15C08-23a

PERMANENT LONGITUDINAL PAVEMENT MARKINGS

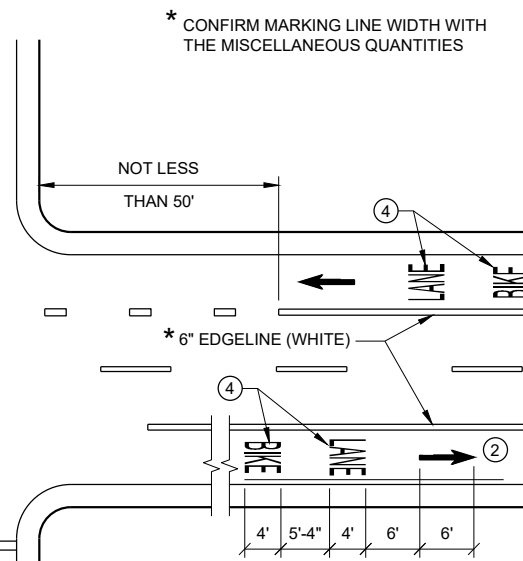
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE May 2023 /S/ Jeannie Silver
STATEWIDE SIGNING AND MARKING ENGINEER

FHWA



DESIGNATED BIKE LANE - NO PARKING

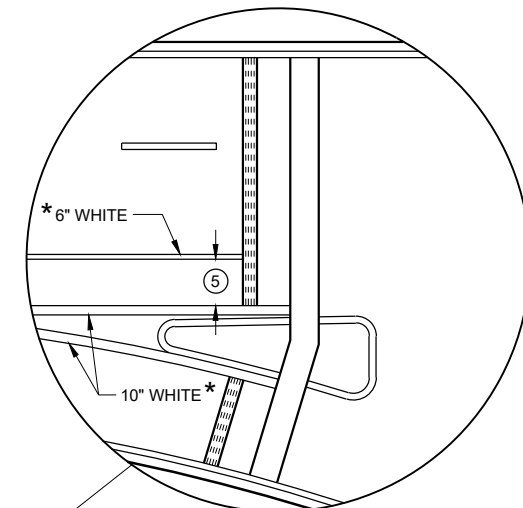
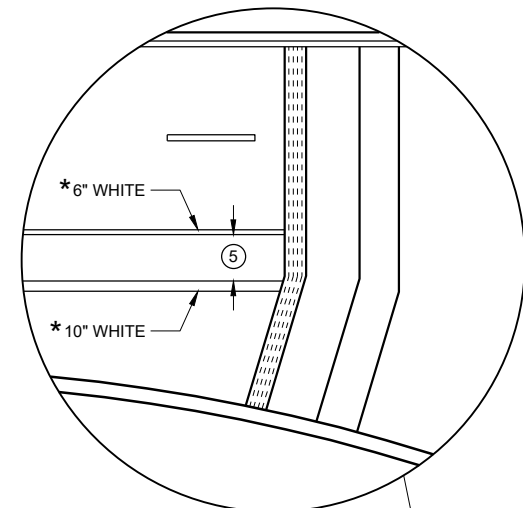


4 LANE DIVIDED WITHOUT ISLAND

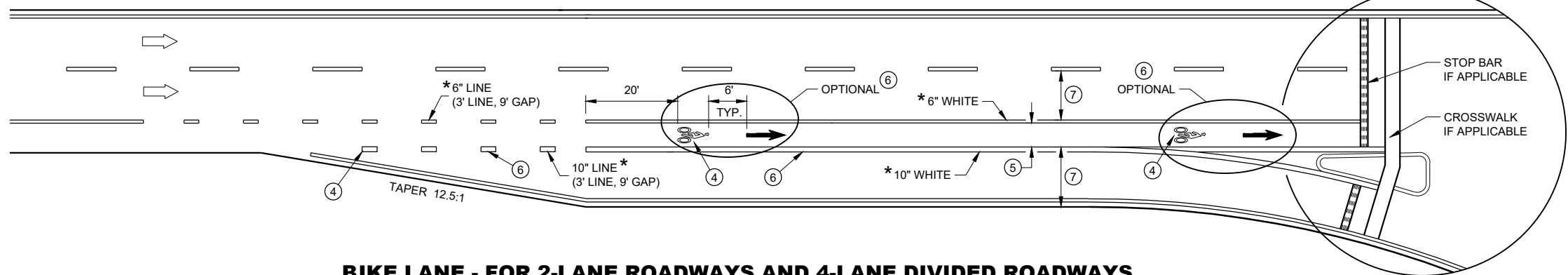
GENERAL NOTES

- ① DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.
- ② MINIMUM OF ONE PER BLOCK. MAXIMUM OF 250 FEET.
- ③ DOTTED LINES (3' LINE, 9' GAP) SHOULD BE USED 50 FEET TO 200 FEET IN ADVANCE OF AN INTERSECTION WHERE THERE IS NO RIGHT TURN ONLY LANE AND THERE IS HEAVY RIGHT TURN TRAFFIC OR THERE IS A NEAR-SIDE BUS STOP. AT OTHER INTERSECTIONS WHERE RIGHT TURN TRAFFIC IS LIGHT TO MODERATE, A SOLID LINE CAN BE USED UP TO THE INTERSECTION.
- ④ BIKE SYMBOLS OR WORDS MAY BE USED.
- ⑤ BIKE ACCOMMODATION IS TYPICAL 5 FEET WIDE AND MINIMUM OF 4 FEET FROM A LONGITUDINAL JOINT. USE 5 FEET AT ≥ 45 MPH.
- ⑥ OMIT THESE MARKINGS FOR WIDER TURN LANE APPLICATIONS (MINIMUM OF 15 FOOT WIDE TURN LANE).
- ⑦ REFER TO CONTRACT PLANS FOR LANE WIDTH.

DIRECTION OF TRAVEL →



4 LANE DIVIDED WITH ISLAND

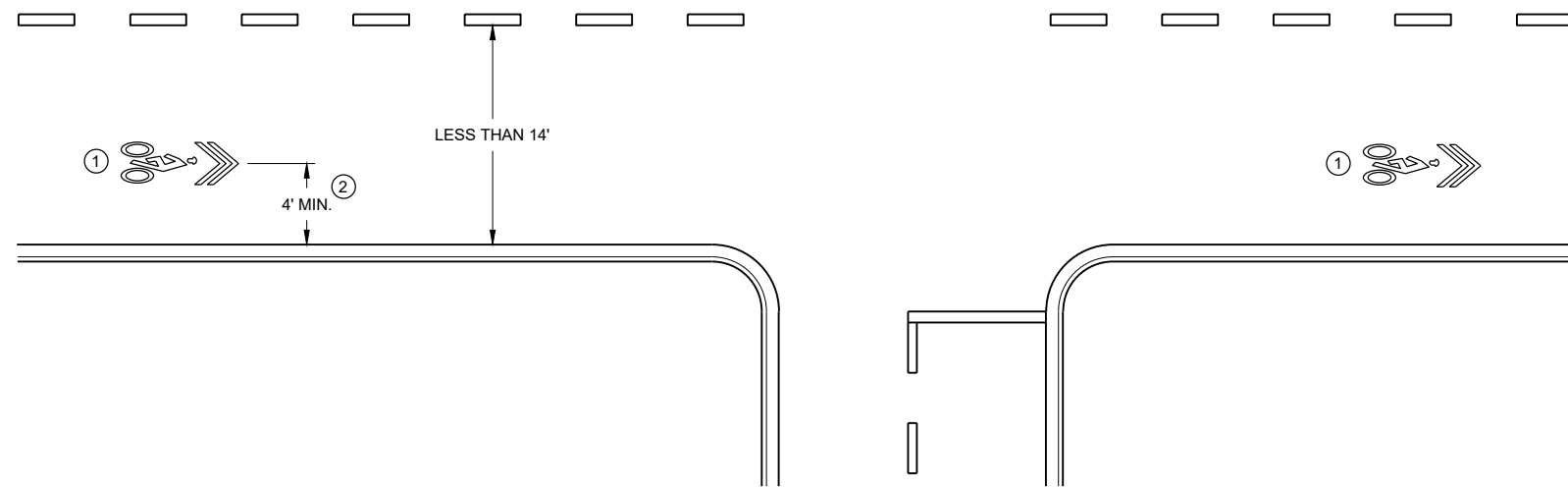


BIKE LANE - FOR 2-LANE ROADWAYS AND 4-LANE DIVIDED ROADWAYS (4-LANE DIVIDED WITH RIGHT TURN LANE SHOWN)

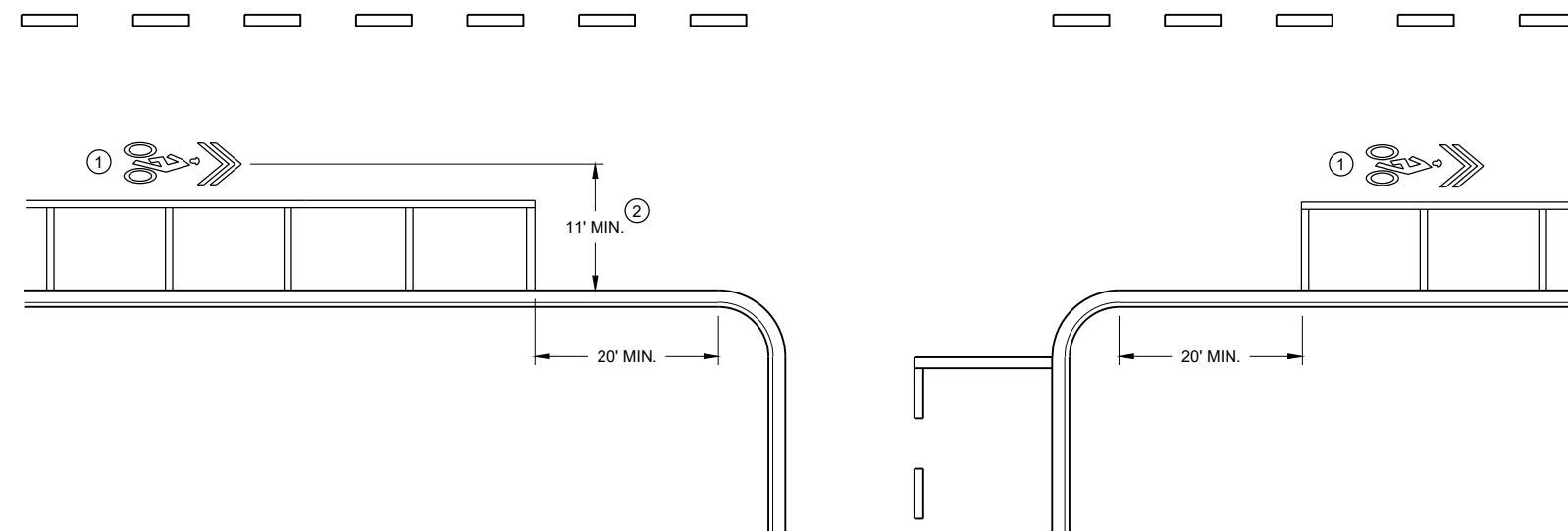
BIKE LANE MARKING	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2023 DATE	/S/ Matthew Rauch STATE SIGNING AND MARKING ENGINEER
<small>FHWA</small>	

GENERAL NOTES

- ① MINIMUM OF ONE PER BLOCK, MAXIMUM OF 250 FEET.
- ② OR TO EDGE OF PAVEMENT WITHOUT CURB.



WITHOUT PARKING



WITH PARKING

6

6

SDD 15C29-08b

SDD 15C29-08b

**PAVEMENT MARKING
FOR SHARED LANE
35 MPH OR LESS**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

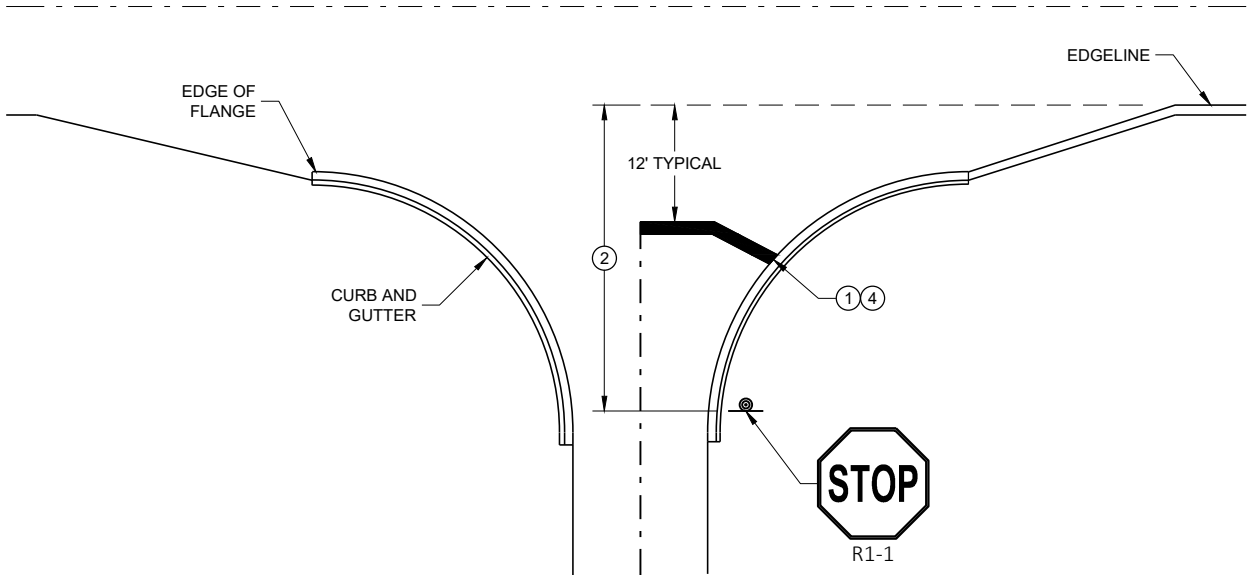
APPROVED
May 2023 /S/ Matthew Rauch
DATE STATE SIGNING AND MARKING
ENGINEER

FHWA

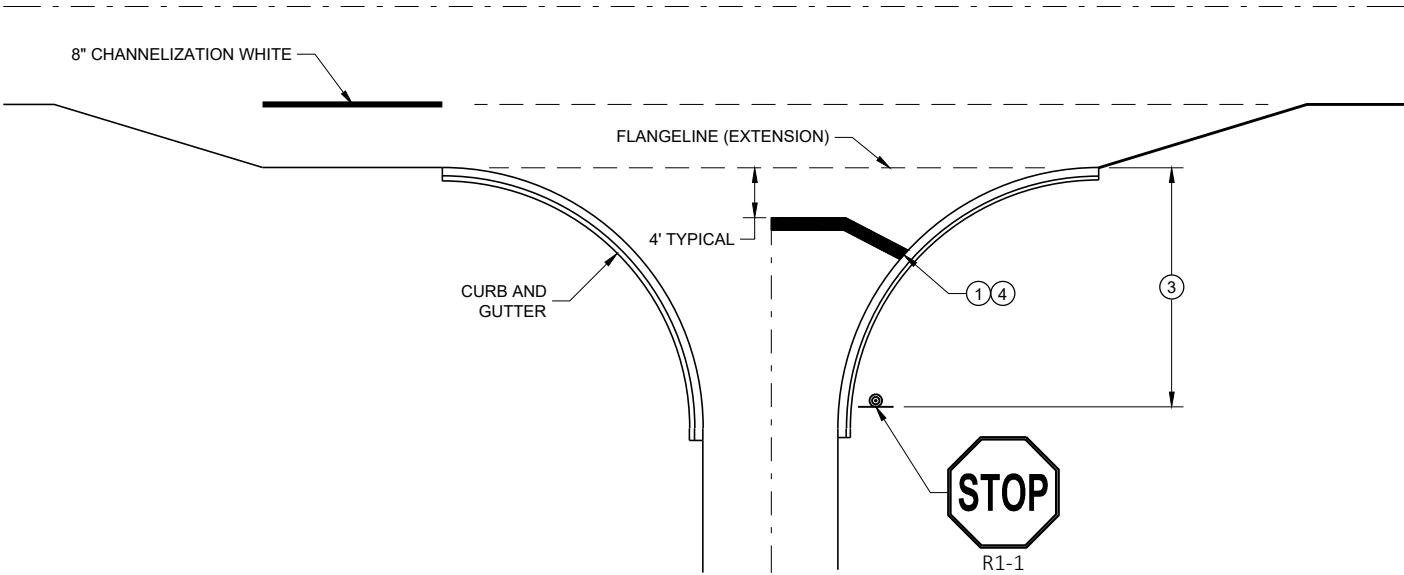
GENERAL NOTES

STOP SIGN SHALL BE PLACED A MINIMUM OF 6 FEET TO A MAXIMUM OF 50 FEET FROM THE EDGELINE LOCATION.

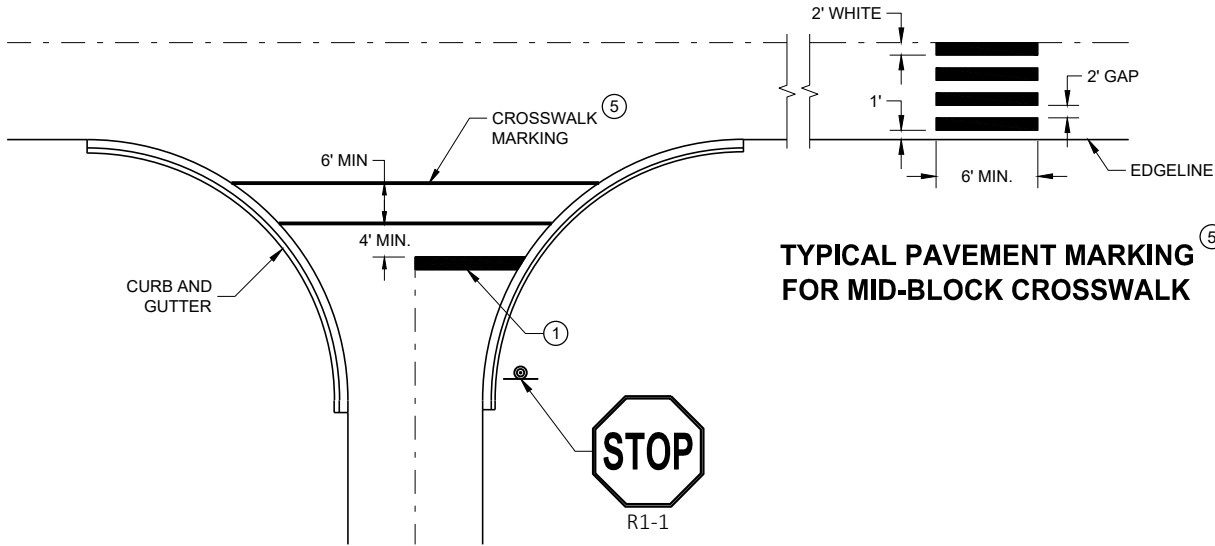
- ① 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE REGION MARKING ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
- ② NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGELINE.
- ③ NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGELINE EXTENSION.
- ④ MOVE CLOSER TO THE EDGE OF TRAVEL LINE AS NEEDED FOR VISIBILITY AND SIGHT LINES (NO CLOSER THAN 4 FEET).
- ⑤ LADDER BAR CROSSWALKS SHOULD ONLY BE USED FOR MID BLOCK CROSSINGS. USE 2 - 6" TRANSVERSE LINES INSTEAD.



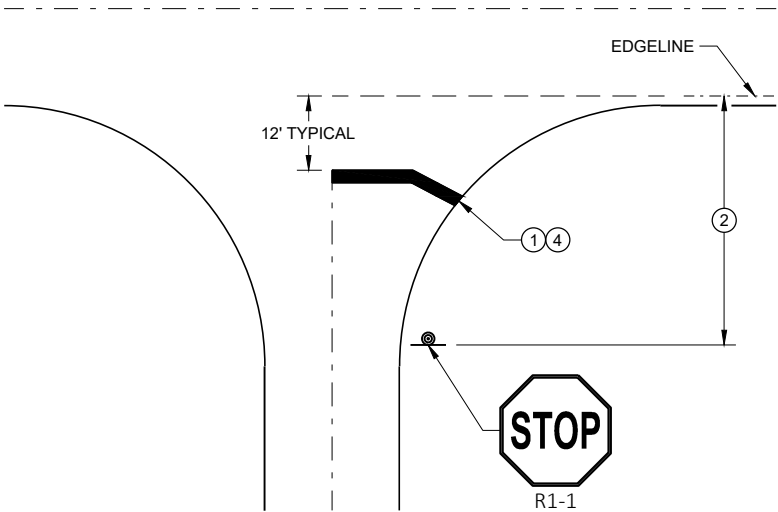
TYPICAL STOP LINE PAVEMENT MARKING WITH CURB AND GUTTER



TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH RIGHT TURN LANE



TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH CROSSWALK MARKING



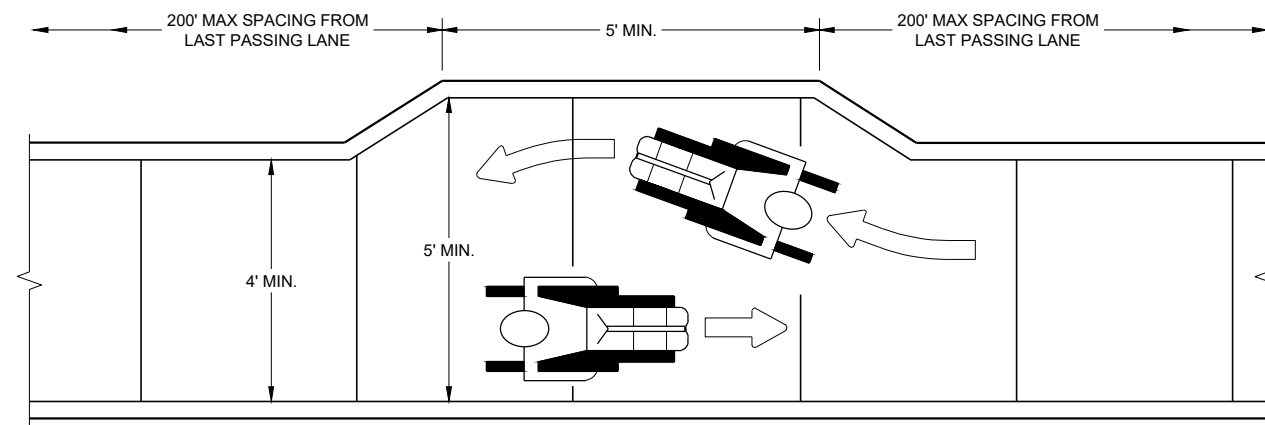
TYPICAL STOP LINE PAVEMENT MARKING WITHOUT CURB AND GUTTER

STOP LINE AND CROSSWALK PAVEMENT MARKING

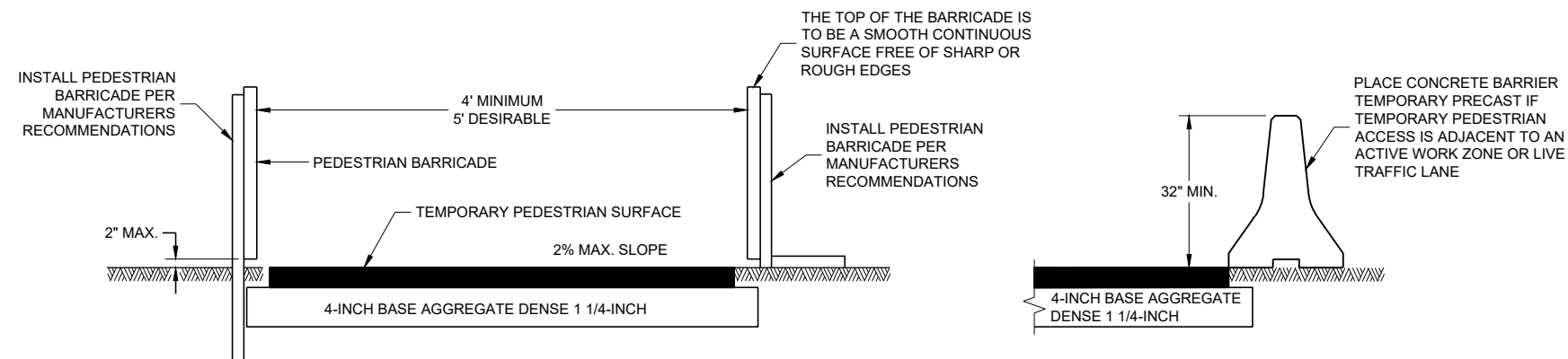
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2019 /S/ Matthew Rauch
DATE STATE SIGNING AND MARKING ENGINEER

FHWA



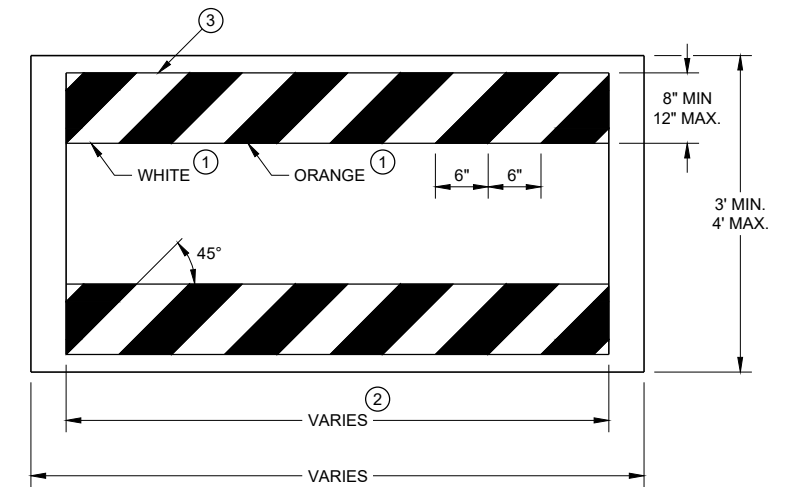
NARROW SIDEWALK PASSING DETAIL



TEMPORARY PEDESTRIAN ACCESS

GENERAL NOTES

- BARRICADE DEVICE SELECTED FROM APPROVED PRODUCT LIST
- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② SHEETING REQUIRED ON MORE THAN 50% OF BARRICADE WIDTH.
- ③ PLACE SHEETING ON BOTH SIDES OF THE BARRICADE.
- * USE THIS DETAIL FOR SHEETING PLACEMENT REFERENCE.

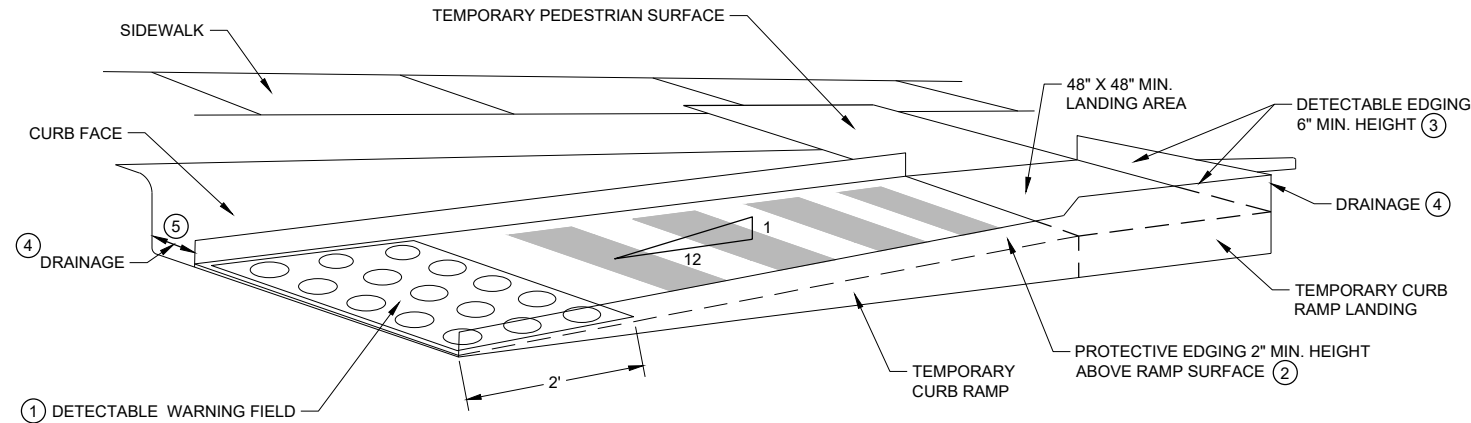


TEMPORARY PEDESTRIAN BARRICADE*

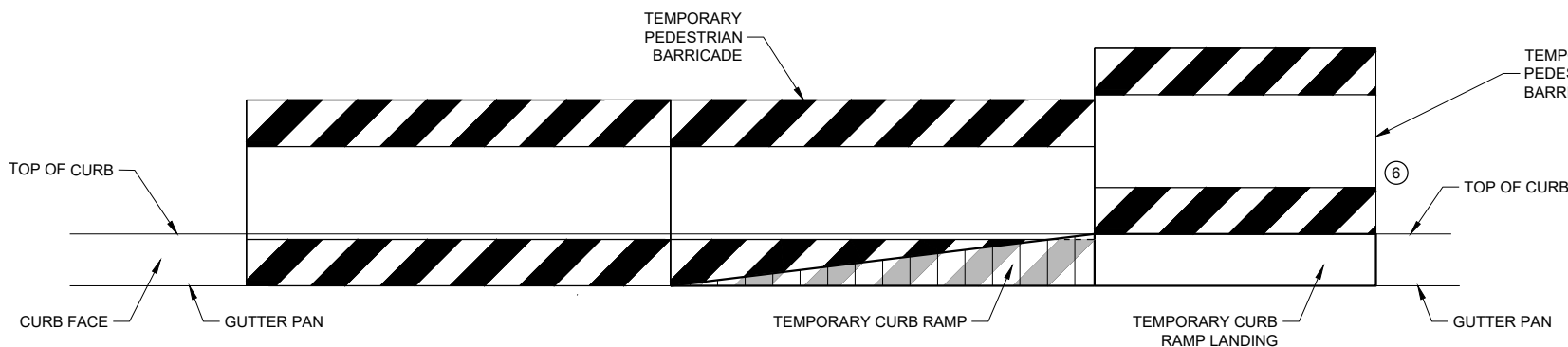
GENERAL NOTES

CURB RAMPS SHALL BE 48" MIN. WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE.
 CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.
 CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.
 LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.
 CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES MAY BE VERTICAL UP TO 1/4" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".

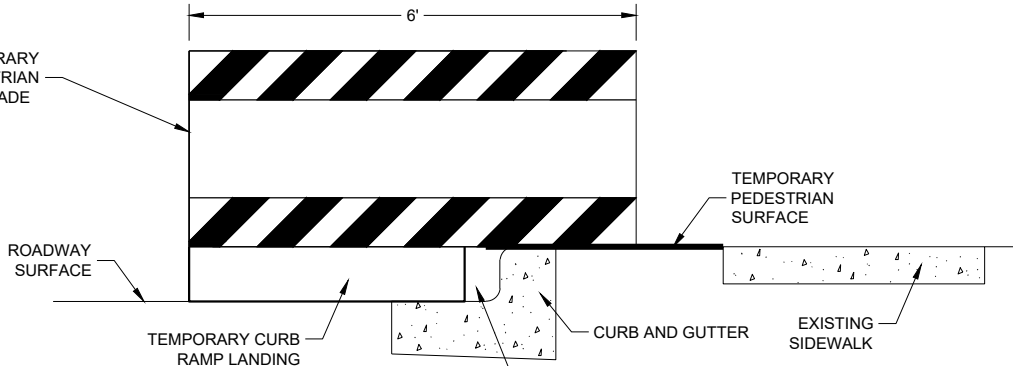
- ① INSTALL CONTRASTING TEMPORARY DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS, AS SHOWN IN THE PLANS.
- ② PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- ③ DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- ④ DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ⑤ ENSURE CURB RAMP IS OUT OF THE GUTTER PAN.
- ⑥ IF ONLY PART OF THE END PANEL OF TEMPORARY PEDESTRIAN BARRICADE PANEL IS NEEDED, EXTEND EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL HERE.



PERSPECTIVE VIEW



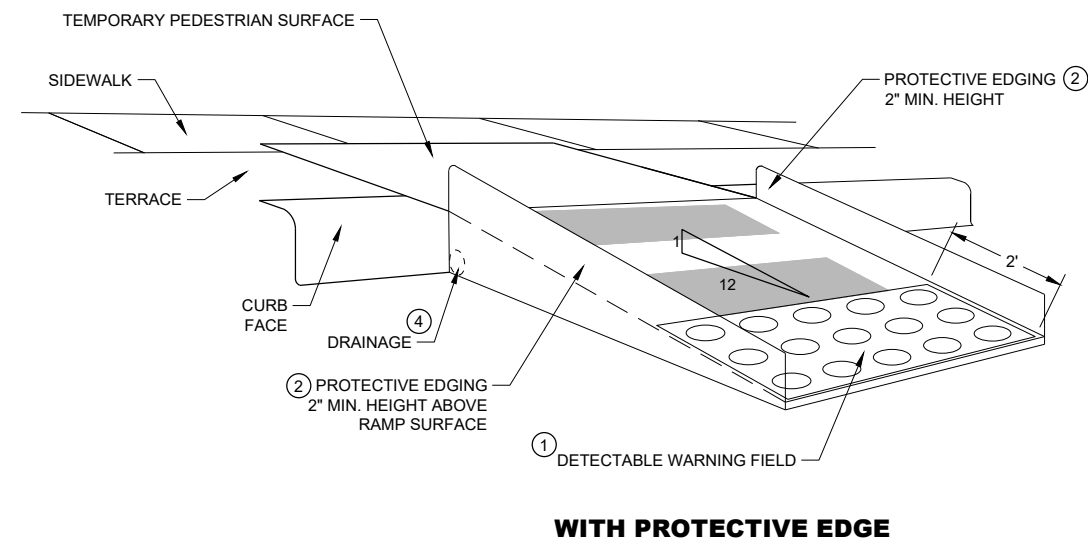
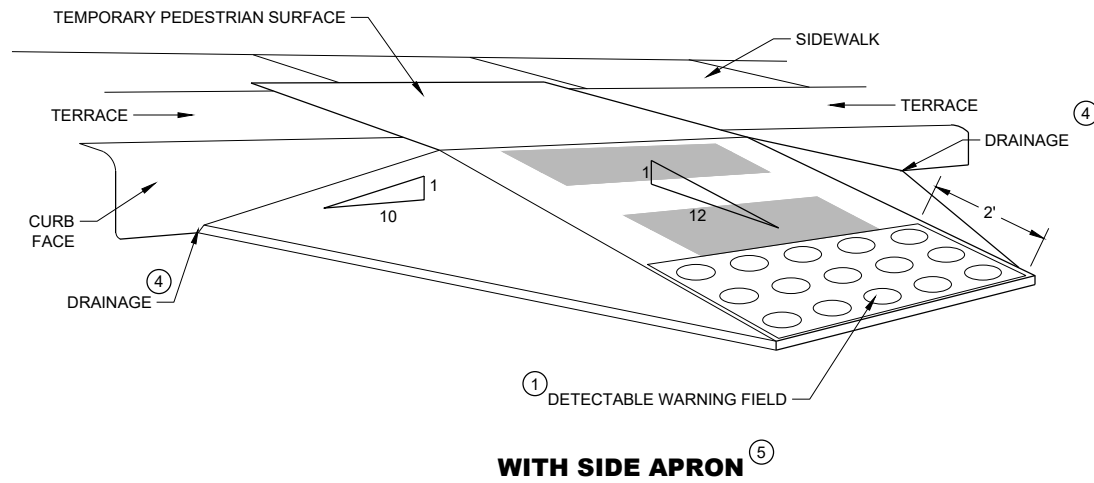
FRONT VIEW



SIDE VIEW

TEMPORARY CURB RAMP PARALLEL TO CURB

<p>TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION</p>
<p>STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION</p>



TEMPORARY CURB RAMP PERPENDICULAR TO CURB

GENERAL NOTES

CURB RAMPS SHALL BE 48" MINIMUM WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE.

ALTERNATE SIDEWALK WORK BETWEEN LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.

CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.

CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.

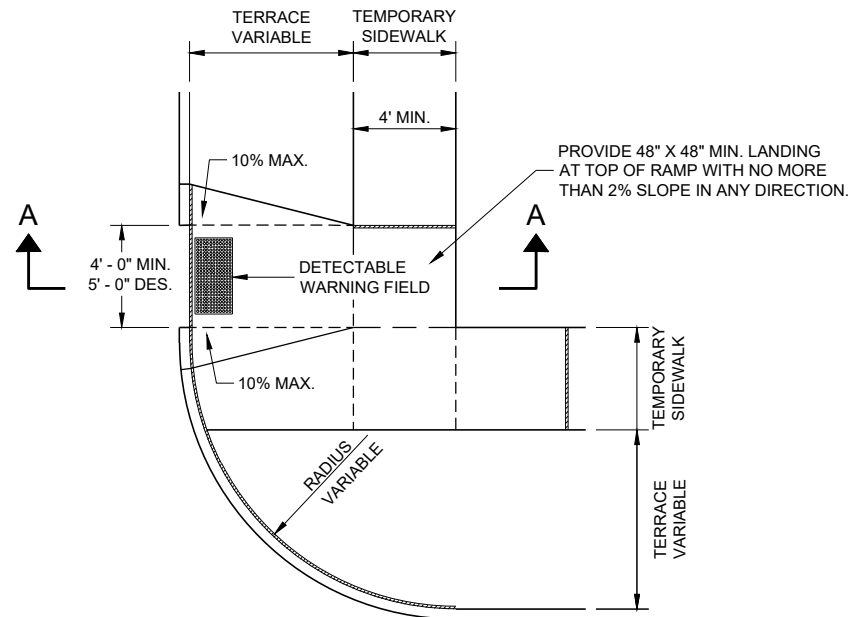
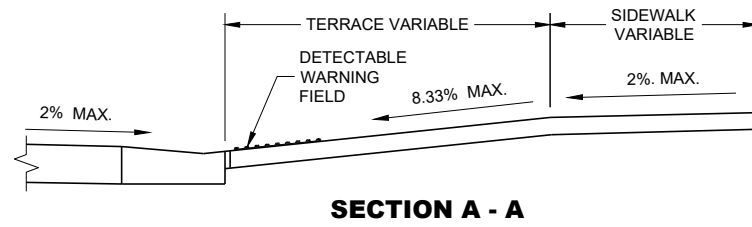
LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.

CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES MAY BE VERTICAL UP TO 1/4" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".

- (1) INSTALL CONTRASTING TEMPORARY DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS, AS SHOWN IN THE PLANS
- (2) PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- (3) DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- (4) DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- (5) CAN ONLY BE USED FOR RAMPS WITH 6" OR LESS OF VERTICAL CHANGE.

GENERAL NOTES

- BARRICADE DEVICE SELECTED FROM APPROVED PRODUCT LIST
- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② SHEETING REQUIRED ON MORE THAN 50% OF BARRICADE WIDTH.
- ③ PLACE SHEETING ON BOTH SIDES OF THE BARRICADE.
- ★ USE THIS DETAIL FOR SHEETING PLACEMENT REFERENCE.



PLAN VIEW
TEMPORARY TYPE 3 RAMP
 (OUTSIDE OF CROSSWALK AREA)

6

6

SDD 15D30-09d

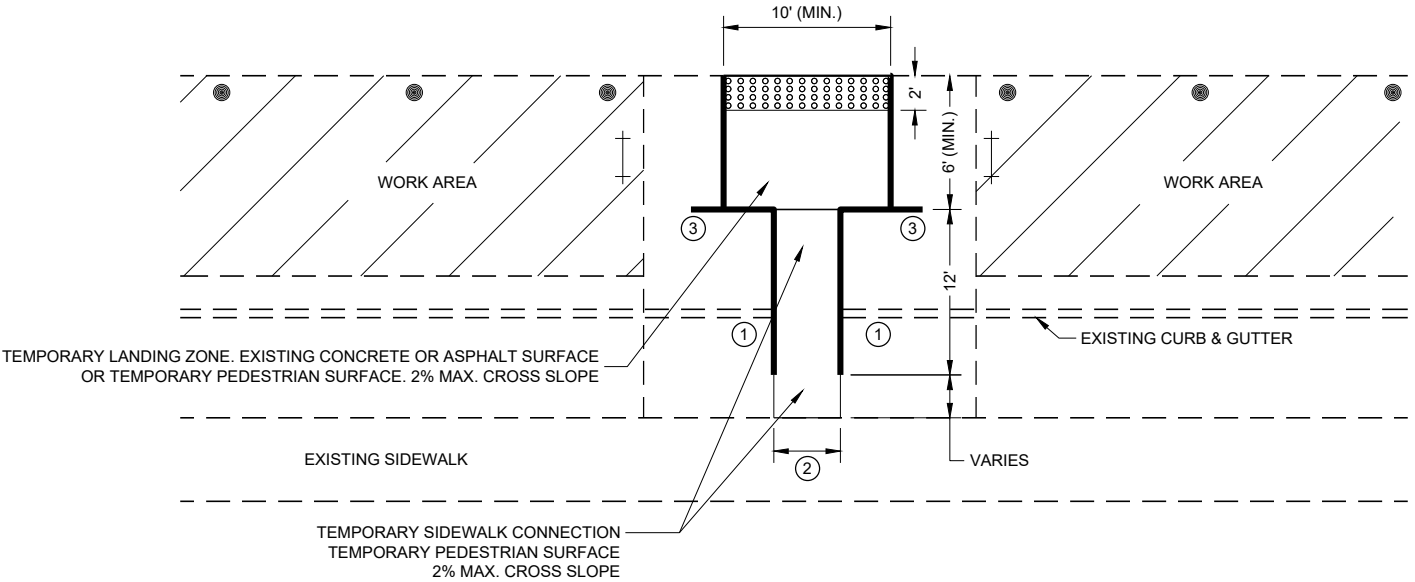
SDD 15D30-09d

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2023 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
<small>FHWA</small>	

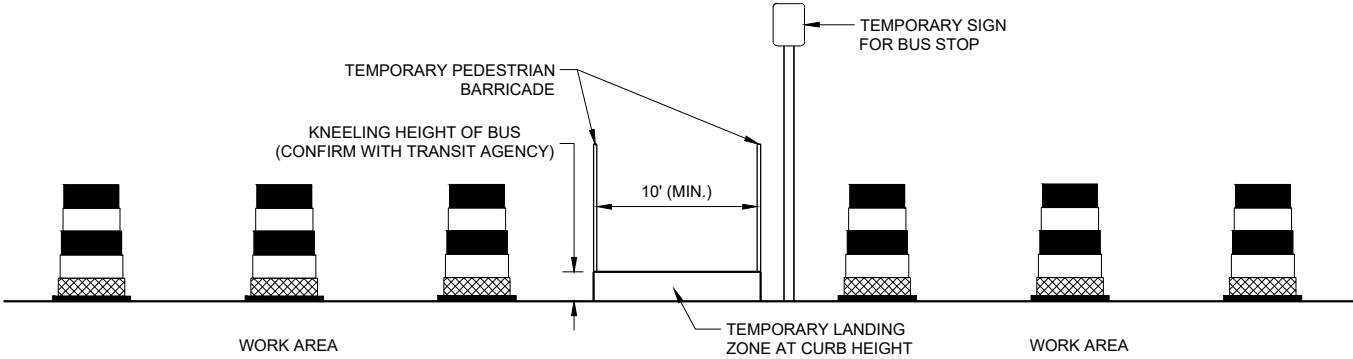
GENERAL NOTES

- TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.
- NOTIFY THE BUS COMPANY 7 DAYS IN ADVANCE OF THE BUS STOP RELOCATION.
- PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMP OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.
- CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES MAY BE VERTICAL UP TO 1/4" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".
- CURB RAMP AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.

- ① DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ② 5' WIDE MIN. WITH TEMPORARY PEDESTRIAN BARRICADE, 10' WIDE MIN. WITHOUT TEMPORARY PEDESTRIAN BARRICADE.
- ③ PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE INTO THIS SPACE.



PLAN VIEW



**PROFILE VIEW
TEMPORARY BUS STOP PAD**







LEGEND

	TRAFFIC CONTROL DRUM
	TYPE III BARRICADE
	TEMPORARY PEDESTRIAN BARRICADE
	TEMPORARY DETECTABLE WARNING FIELD
	WORK AREA

**TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION**

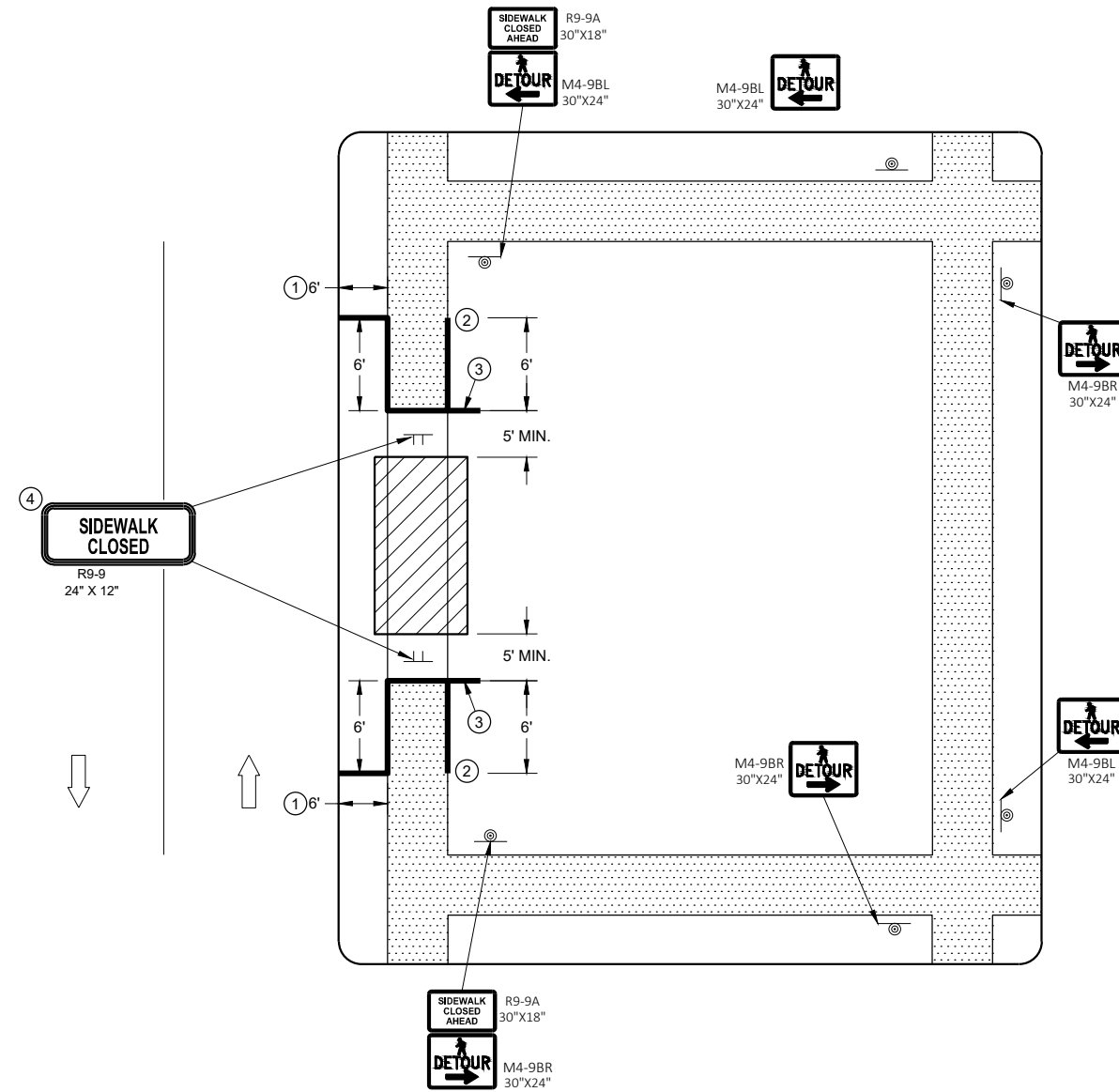
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  SIGN ON TEMPORARY SUPPORT
-  UNDER PEDESTRIAN TRAFFIC
-  WORK AREA
-  TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC

GENERAL NOTES








- SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.
- WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.
- SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.
- PLACE TEMPORARY PEDESTRIAN BARRICADE TO FIT FIELD CONDITIONS, AVOIDING CONFLICTS WITH DRIVEWAYS AND OTHER EXISTING FEATURES.
- ① IF TERRACE IS LESS THAN 6 FEET WIDE, OMIT TEMPORARY PEDESTRIAN BARRICADE FROM THE SIDEWALK TO THE CURB.
 - ② PLACE BARRICADE CLOSURE SO THAT THE TEMPORARY PEDESTRIAN BARRICADE END IS AT THE LAST OPEN SIDEWALK ACCESS TO RESIDENCES OR BUSINESSES BEFORE THE SIDEWALK CLOSURE.
 - ③ IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
 - ④ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.



SIDEWALK DETOUR, SIDEWALK ONLY ON ONE SIDE

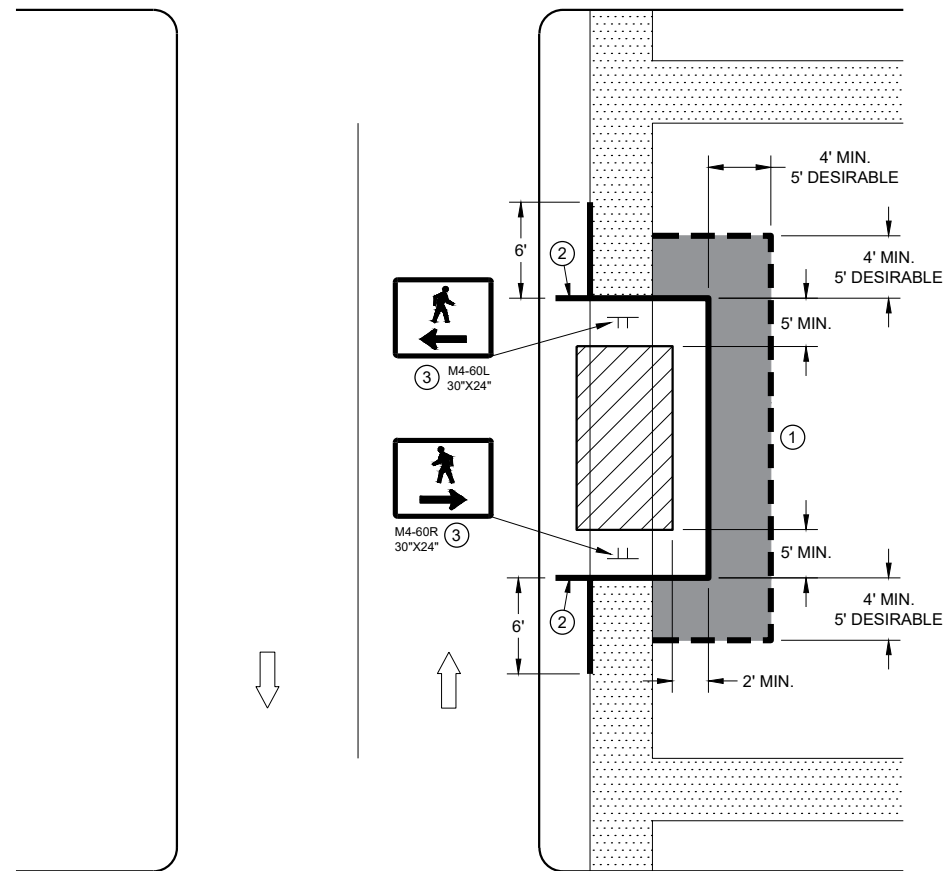
TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  WORK AREA
-  UNDER PEDESTRIAN TRAFFIC
-  TEMPORARY PEDESTRIAN SURFACE
-  TEMPORARY PEDESTRIAN BARRICADE
-  OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC










GENERAL NOTES

- TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.
- SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.
- WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.
- SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.
- ① USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
 - ② IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
 - ③ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.



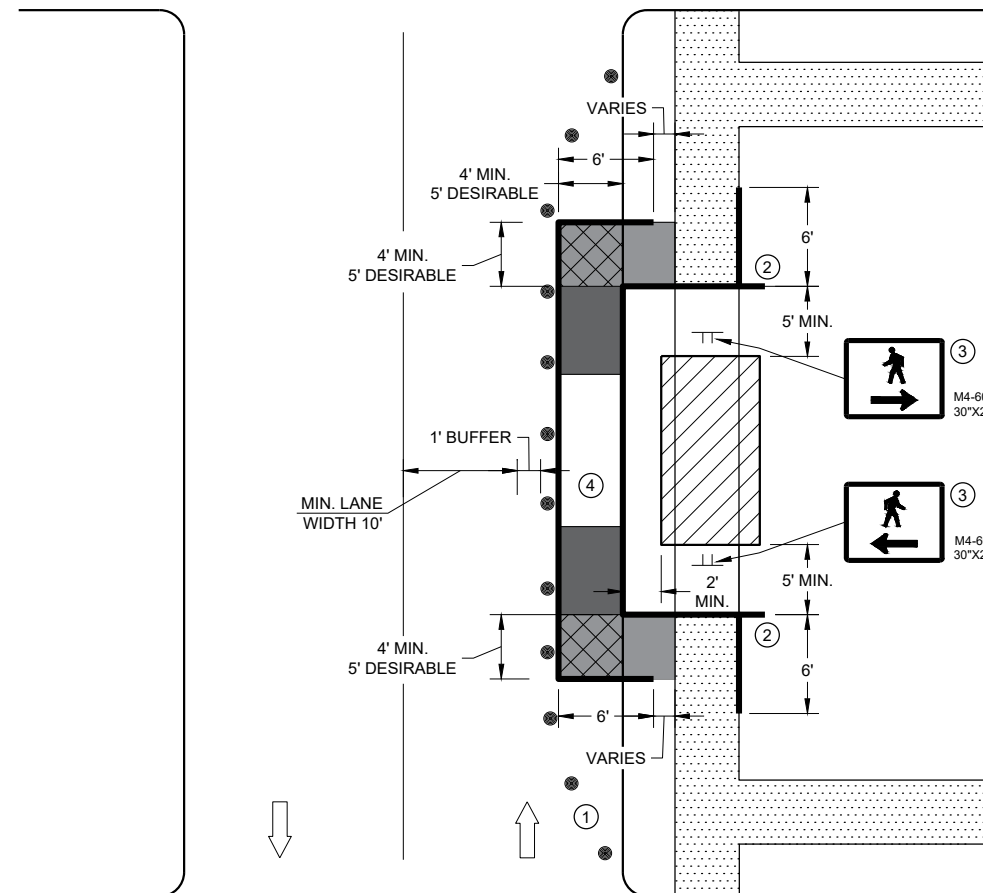
**SIDEWALK DIVERSION
SINGLE SIDE**

LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL DRUM
-  WORK AREA
-  UNDER PEDESTRIAN TRAFFIC
-  TEMPORARY CURB RAMP
-  TEMPORARY PEDESTRIAN SURFACE "A"
-  TEMPORARY PEDESTRIAN SURFACE "B"
-  TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC

GENERAL NOTES

- TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.
- SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.
- WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.
- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND BUFFER SPACE REQUIRED.
 - ② PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL PAST THE SIDEWALK ON THE SIDE AWAY FROM THE ROAD.
 - ③ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.
 - ④ USE EXISTING PAVEMENT SURFACE. IF EXISTING PAVEMENT SURFACE HAS BEEN REMOVED, USE A TEMPORARY PEDESTRIAN SURFACE.



SIDEWALK DIVERSION, SINGLE SIDE

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

IF PEDESTRIAN PUSH BUTTONS ARE PRESENT ON THE EXISTING FACILITY, ENSURE THEY ARE MAINTAINED/ACCESSIBLE FOR PEDESTRIAN USE THROUGHOUT THE TEMPORARY PEDESTRIAN ACCOMMODATIONS.











SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

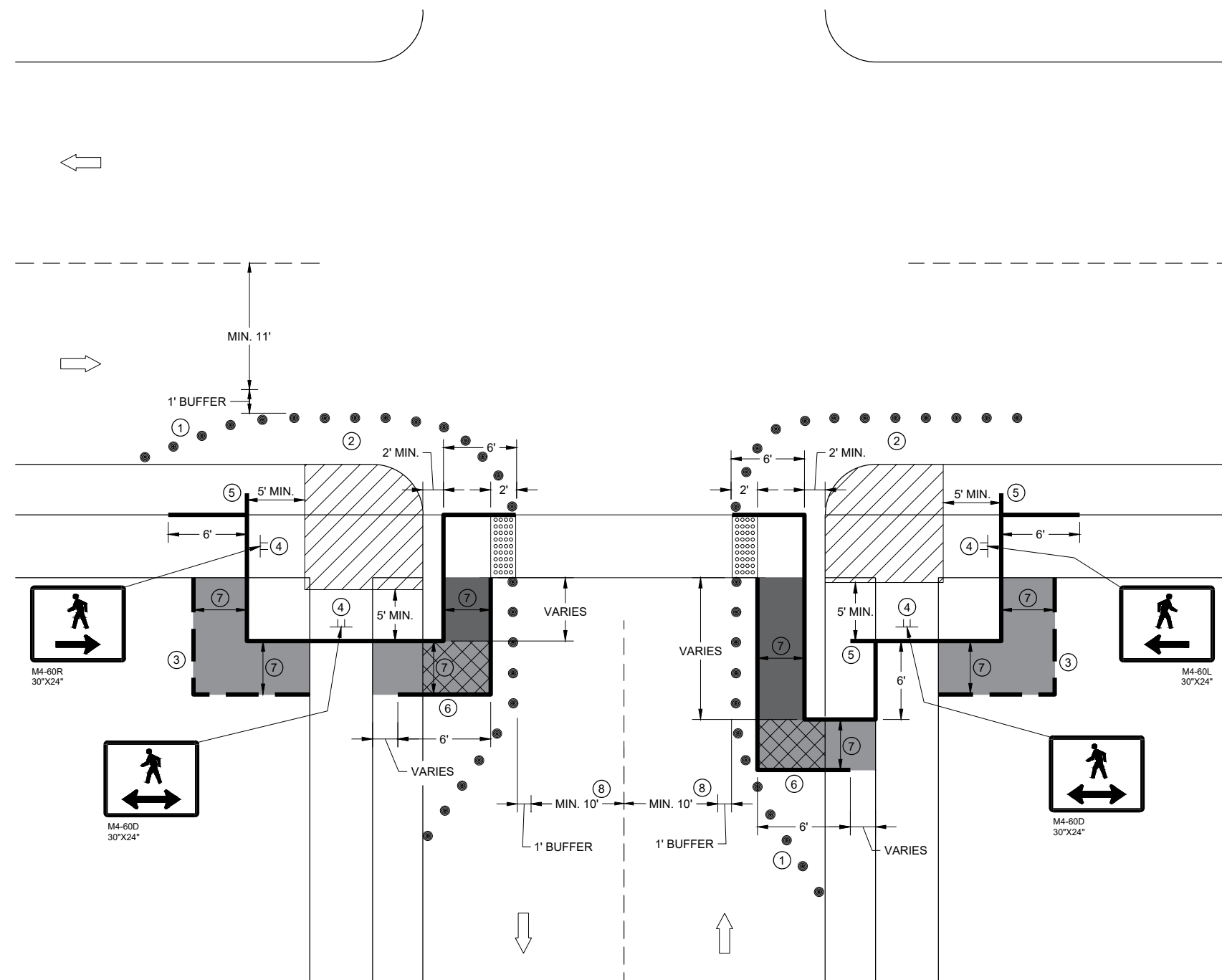
TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG

WHEN TEMPORARY PEDESTRIAN BARRICADE RUNS PARALLEL ALONG THE SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND PROPER BUFFER SPACE REQUIRED.
- ② PROVIDE ADEQUATE SPACE FOR CONTRACTOR OPERATIONS
- ③ USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
- ④ MOUNTING HEIGHT OF 5 FEET FROM SIDEWALK SURFACE TO BOTTOM OF SIGN.
- ⑤ PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL IN THE SIDEWALK TERRACE.
- ⑥ IF TEMPORARY PEDESTRIAN BARRICADE DOES NOT REACH THE FACE OF THE CURB, USE AN ADDITIONAL PANEL AND EXTEND INTO THE TERRACE.
- ⑦ 4 FEET MINIMUM, 5 FEET DESIRABLE
- ⑧ IF MINIMUM LANE WIDTHS CAN'T BE ATTAINED, CURB RAMPS MAY NEED TO BE CONSTRUCTED AT SEPARATE TIMES.

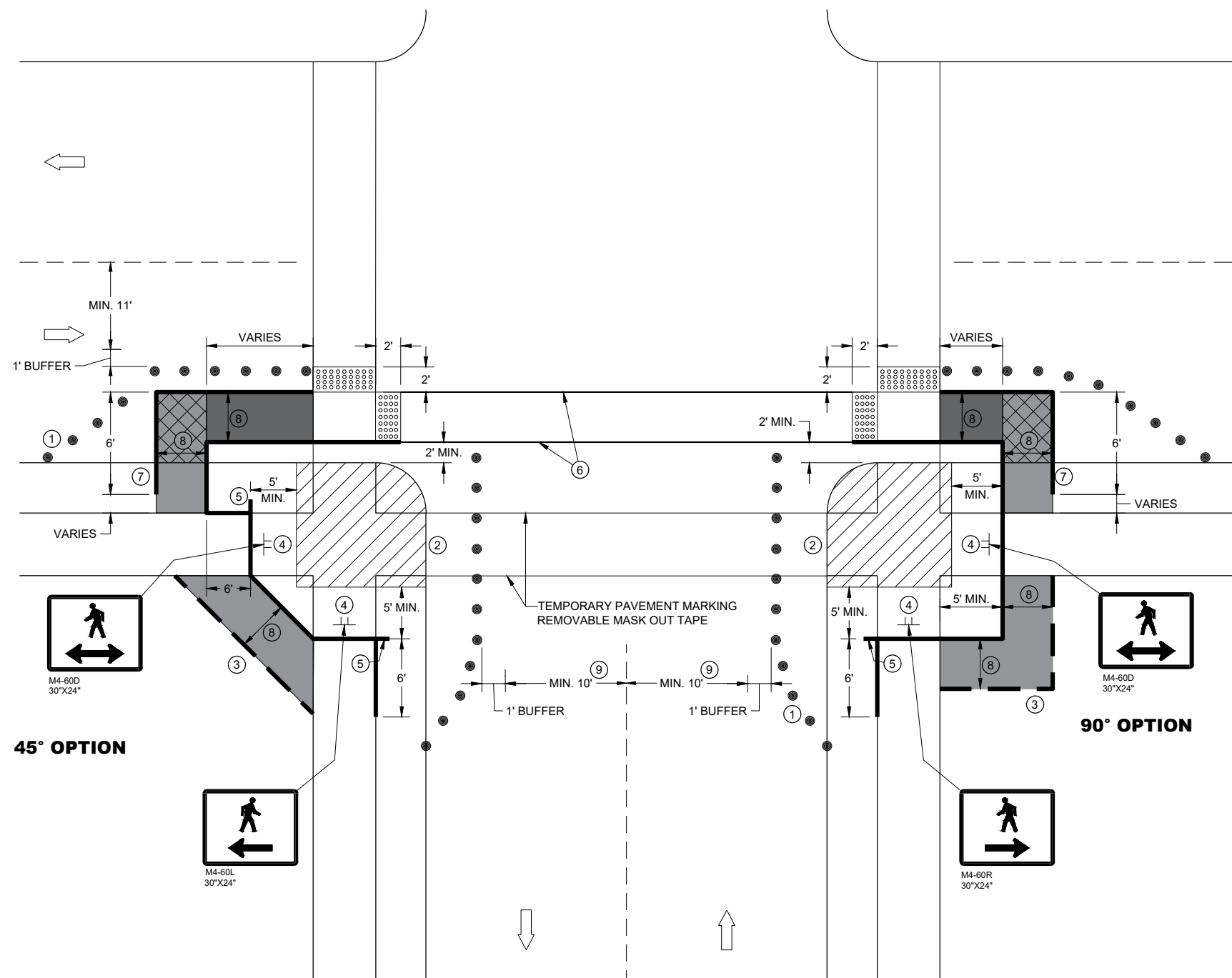
LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL DRUM
-  WORK AREA
-  TEMPORARY CURB RAMP
-  TEMPORARY PEDESTRIAN SURFACE "A"
-  TEMPORARY PEDESTRIAN SURFACE "B"
-  TEMPORARY DETECTABLE WARNING FIELD
-  TEMPORARY PEDESTRIAN BARRICADE
-  OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC



**CURB RAMP PEDESTRIAN TRAFFIC CONTROL
SIDEWALK ON SINGLE SIDE**

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION



CURB RAMP PEDESTRIAN TRAFFIC CONTROL

GENERAL NOTES

IF PEDESTRIAN PUSH BUTTONS ARE PRESENT ON THE EXISTING FACILITY, ENSURE THEY ARE MAINTAINED/ACCESSIBLE FOR PEDESTRIAN USE THROUGHOUT THE TEMPORARY PEDESTRIAN ACCOMMODATIONS.

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG

WHEN TEMPORARY PEDESTRIAN BARRICADE RUNS PARALLEL ALONG THE SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND PROPER BUFFER SPACE REQUIRED.
- ② PROVIDE ADEQUATE SPACE FOR CONTRACTOR OPERATIONS
- ③ USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
- ④ MOUNTING HEIGHT OF 5 FEET FROM SIDEWALK SURFACE TO BOTTOM OF SIGN.
- ⑤ PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL IN THE SIDEWALK TERRACE.
- ⑥ WHITE 6" TEMPORARY PAVEMENT MARKING
- ⑦ IF TEMPORARY PEDESTRIAN BARRICADE DOES NOT REACH THE FACE OF THE CURB, USE AN ADDITIONAL PANEL AND EXTEND INTO THE TERRACE.
- ⑧ 4 FEET MINIMUM, 5 FEET DESIRABLE
- ⑨ IF MINIMUM LANE WIDTHS CAN'T BE ATTAINED, CURB RAMPS MAY NEED TO BE CONSTRUCTED AT SEPARATE TIMES.







LEGEND

- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL DRUM
- WORK AREA
- TEMPORARY CURB RAMP
- TEMPORARY PEDESTRIAN SURFACE "A"
- TEMPORARY PEDESTRIAN SURFACE "B"
- TEMPORARY DETECTABLE WARNING FIELD
- TEMPORARY PEDESTRIAN BARRICADE
- OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
- DIRECTION OF TRAFFIC

**TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION**

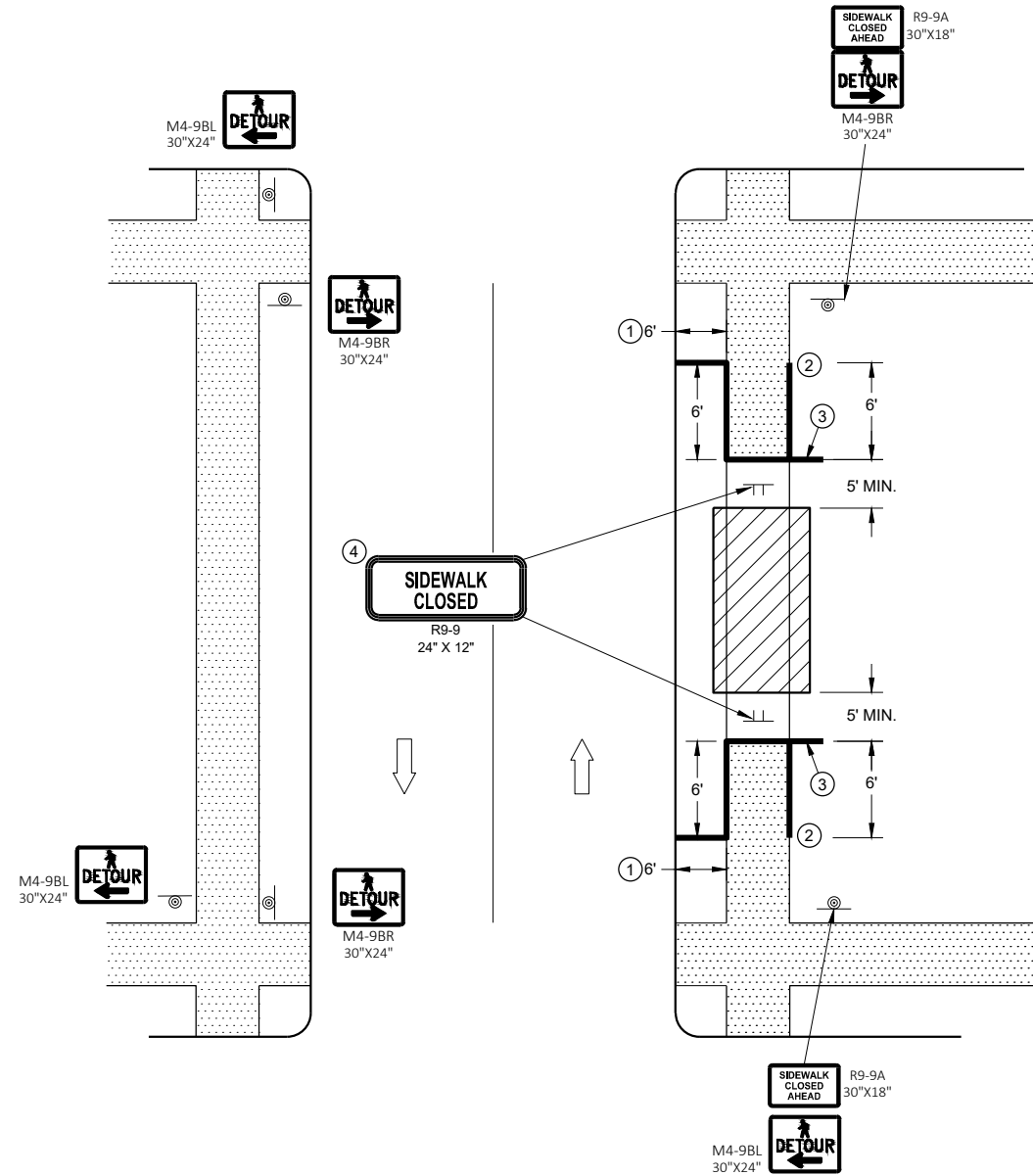
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  SIGN ON TEMPORARY SUPPORT
-  UNDER PEDESTRIAN TRAFFIC
-  WORK AREA
-  TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC

GENERAL NOTES

- SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.
- WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.
- SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.
- PLACE TEMPORARY PEDESTRIAN BARRICADE TO FIT FIELD CONDITIONS, AVOIDING CONFLICT WITH DRIVEWAYS AND OTHER EXISTING FEATURES.
- ① IF TERRACE IS LESS THAN 6 FEET WIDE, OMIT TEMPORARY PEDESTRIAN BARRICADE FROM THE SIDEWALK TO THE CURB.
 - ② PLACE BARRICADE CLOSURE SO THAT THE TEMPORARY PEDESTRIAN BARRICADE END IS AT THE LAST OPEN SIDEWALK ACCESS TO RESIDENCES OR BUSINESSES BEFORE THE SIDEWALK CLOSURE.
 - ③ IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
 - ④ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.



SIDEWALK DETOUR, SIDEWALK ON BOTH SIDES

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

6

6

SDD 15D30 - 09k

SDD 15D30 - 09k

GENERAL NOTES

TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.

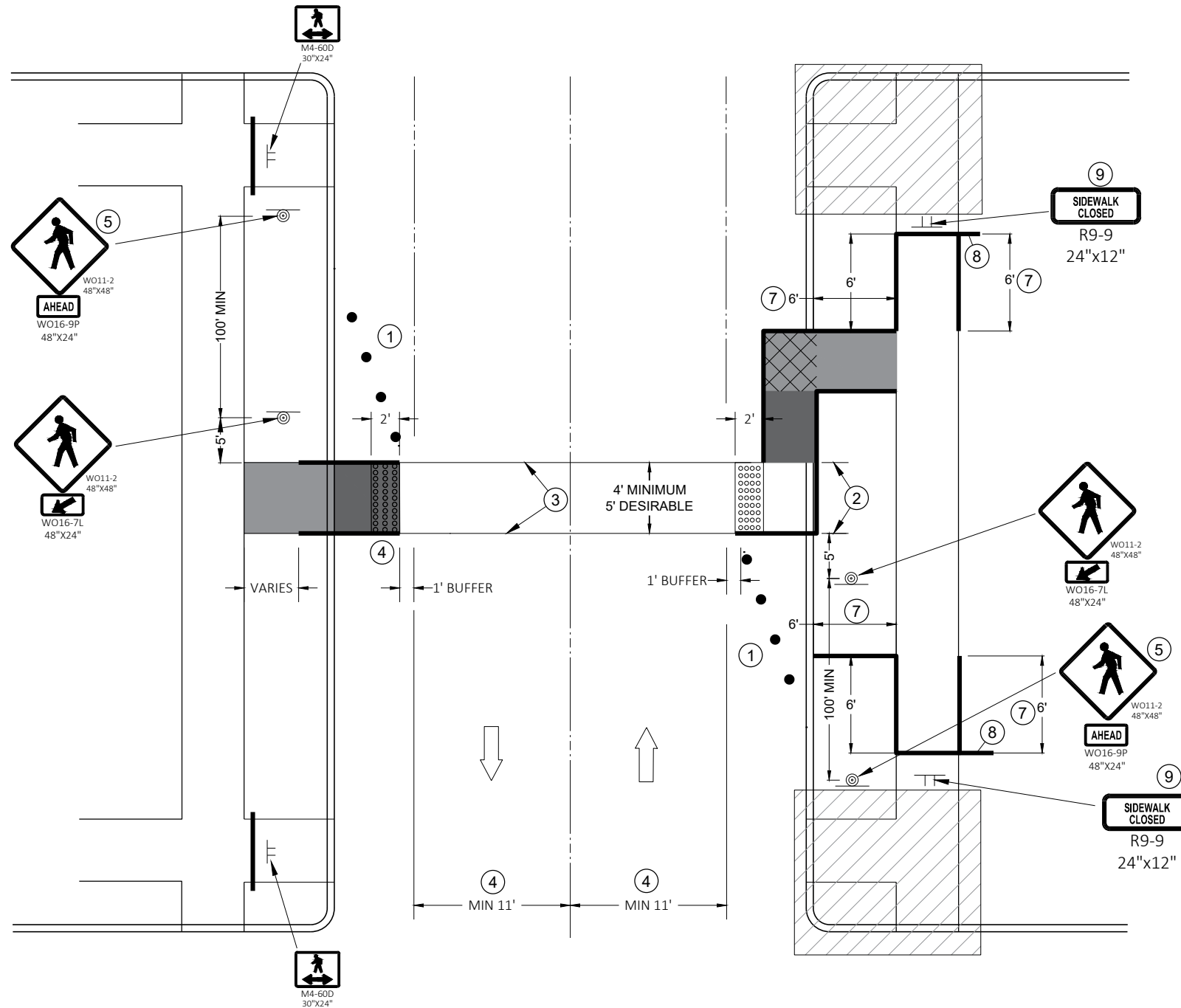
SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

SEE OTHER PEDESTRIAN ACCOMMODATION DETAILS FOR SIGNING AND DEVICES FOR DIFFERENT PEDESTRIAN FACILITIES CLOSURES.

- 1 SHOULDERS OR LANE CLOSURE ADVANCED WARNING AND PROPER BUFFER SPACE REQUIRED.
- 2 4 FEET MINIMUM, 5 FEET DESIRABLE.
- 3 WHITE 6" TEMPORARY PAVEMENT MARKING.
- 4 IF MINIMUM LANE WIDTHS CAN'T BE ATTAINED, PERPENDICULAR CURB RAMP MAY NEED TO BE UTILIZED.
- 5 IF MINIMUM 100' SPACING FROM THE MID-BLOCK CROSSING CANNOT BE ATTAINED BEFORE THE INTERSECTION, REMOVE THIS SIGN ASSEMBLY.
- 6 IF TERRACE IS LESS THAN 6 FEET WIDE, OMIT TEMPORARY PEDESTRIAN BARRICADE FROM THE SIDEWALK TO THE CURB.
- 7 PLACE BARRICADE CLOSURE SO THAT THE TEMPORARY PEDESTRIAN BARRICADE END IS AT THE LAST OPEN SIDEWALK ACCESS TO RESIDENCES OR BUSINESSES BEFORE THE SIDEWALK CLOSURE.
- 8 IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF THE EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
- 9 MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF THE SIGN.

LEGEND

- TRAFFIC CONTROL DRUM
- ┌ SIGN ON TEMPORARY SUPPORT
- ▬ TEMPORARY CURB RAMP
- ◻◻◻◻◻◻◻◻◻◻ TEMPORARY DETECTABLE WARNING FIELD
- ▬ TEMPORARY PEDESTRIAN SURFACE "A"
- ▩ TEMPORARY PEDESTRIAN SURFACE "B"
- ▨ WORK AREA
- ▬ TEMPORARY PEDESTRIAN BARRICADE
- DIRECTION OF TRAFFIC



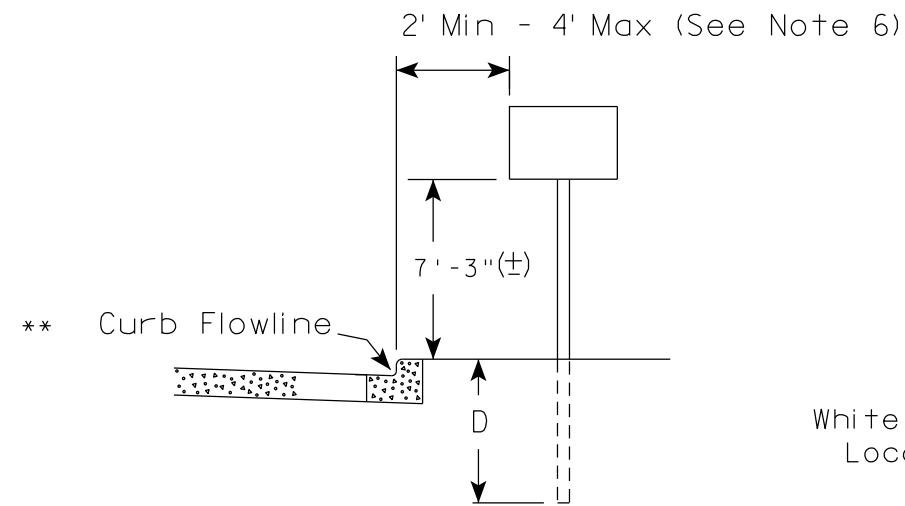
TEMPORARY PEDESTRIAN CROSSING

**TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION**

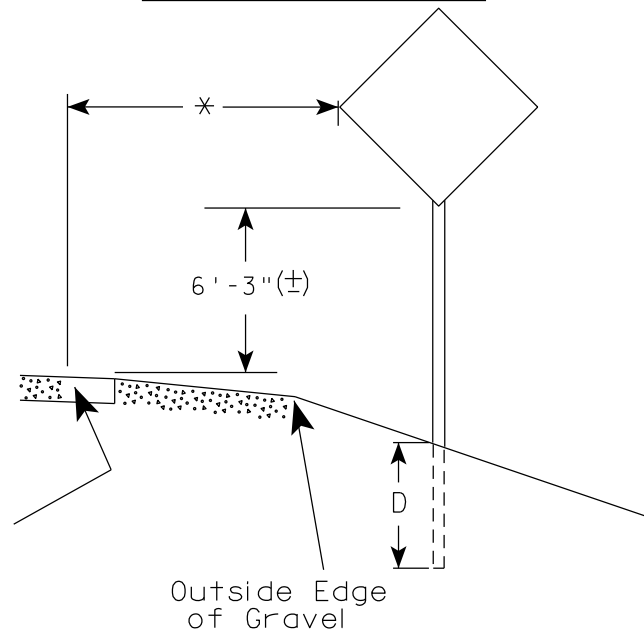
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

URBAN AREA

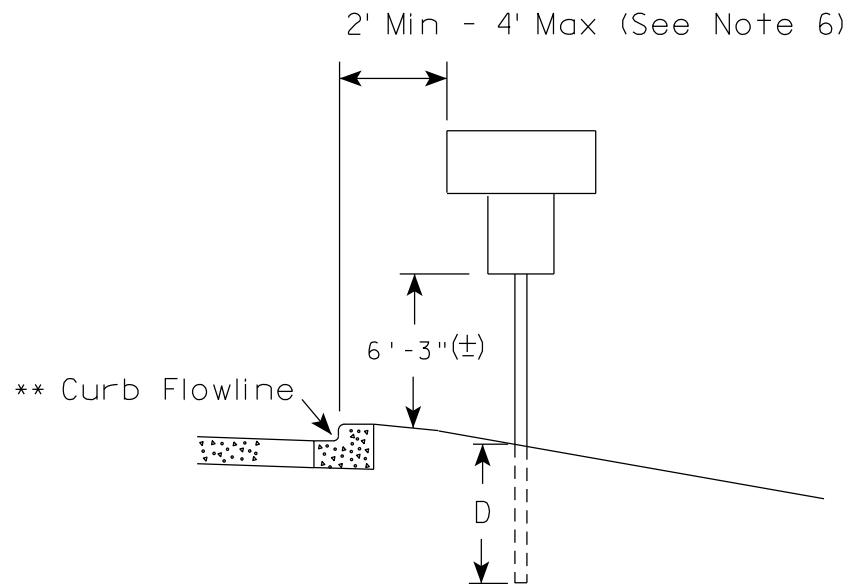
RURAL AREA (See Note 2)



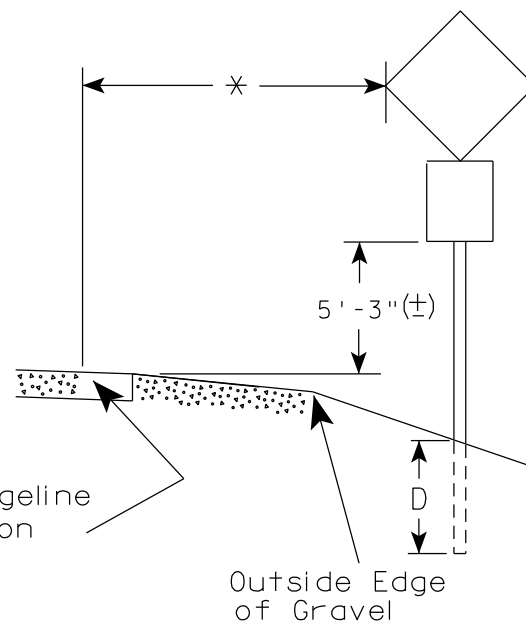
White Edgeline Location



Outside Edge of Gravel



White Edgeline Location



Outside Edge of Gravel

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

GENERAL NOTES

1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on or behind barrier wall, see A4-10 sign plate.
The Double Arrow sign (W12-1D) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).
3. For expressways and freeways, mounting height is 7'- 3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
4. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±).
5. Offset distance shall be consistent with existing signs or consistent throughout length of project.
6. The (±) tolerance for mounting height is 3 inches.
7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the Engineer.

7

7

* * The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 5/13/2020 PLATE NO. A4-3.22



ELEVATION VIEW

DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

- NOTES:**
1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
 2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
 3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



PLAN VIEW

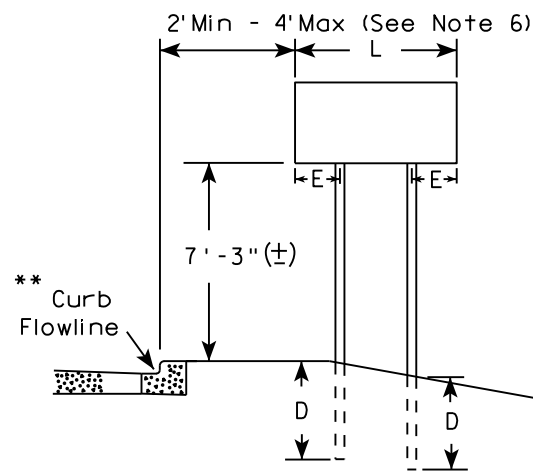
FOR NEW CONCRETE/ ASPHALT INSTALLATIONS

SIGN POST BOX-OUTS A4-3B	
<small>WISCONSIN DEPT OF TRANSPORTATION</small>	
APPROVED <i>Matthew R. Rauch</i> <small>for State Traffic Engineer</small>	
<small>DATE 1/27/14</small>	<small>PLATE NO. A4-3B.1</small>

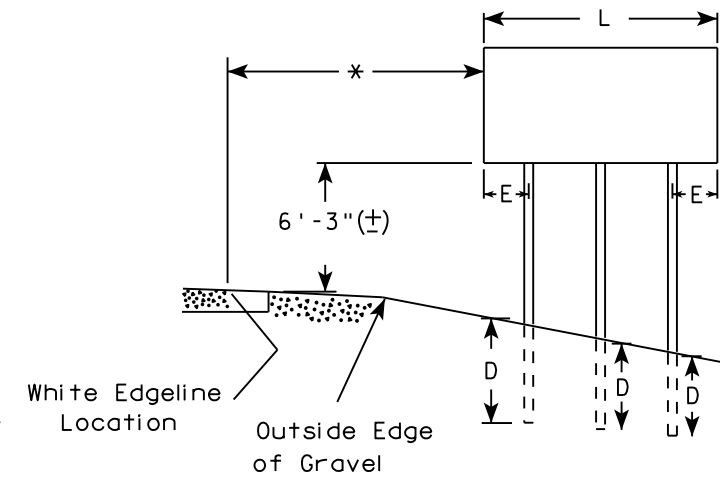
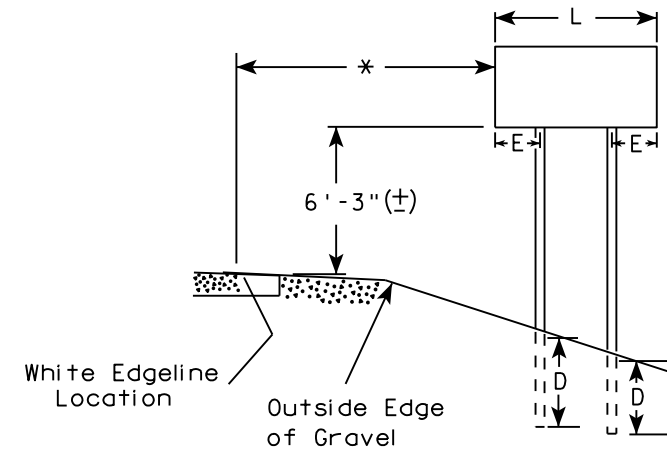
GENERAL NOTES

1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
2. See tables below for required number of posts.
3. For expressways and freeways, mounting height is 7'-3" (±) or 6'-3" (±) depending upon existence of sub-sign.
4. The (±) tolerance for mounting height is 3 inches.
5. J-Assemblies are considered to be one sign for mounting height.
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the engineer.
8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).

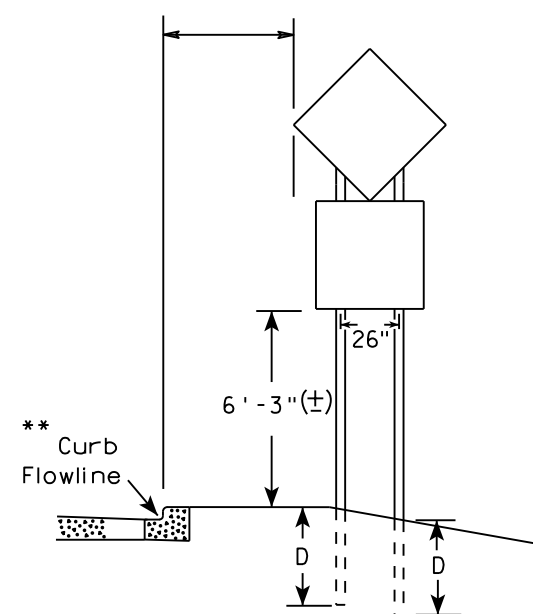
URBAN AREA



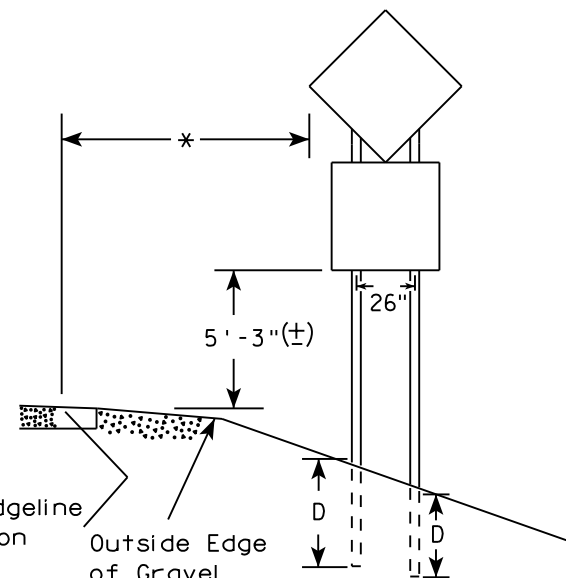
RURAL AREA (See Note 3)



2' Min - 4' Max (See Note 6)



48" DIAMOND WARNING SIGN



48" DIAMOND WARNING SIGN

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

*** See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)	
L	E
Greater than 48" Less than 60"	12"
60" to 108"	L/5

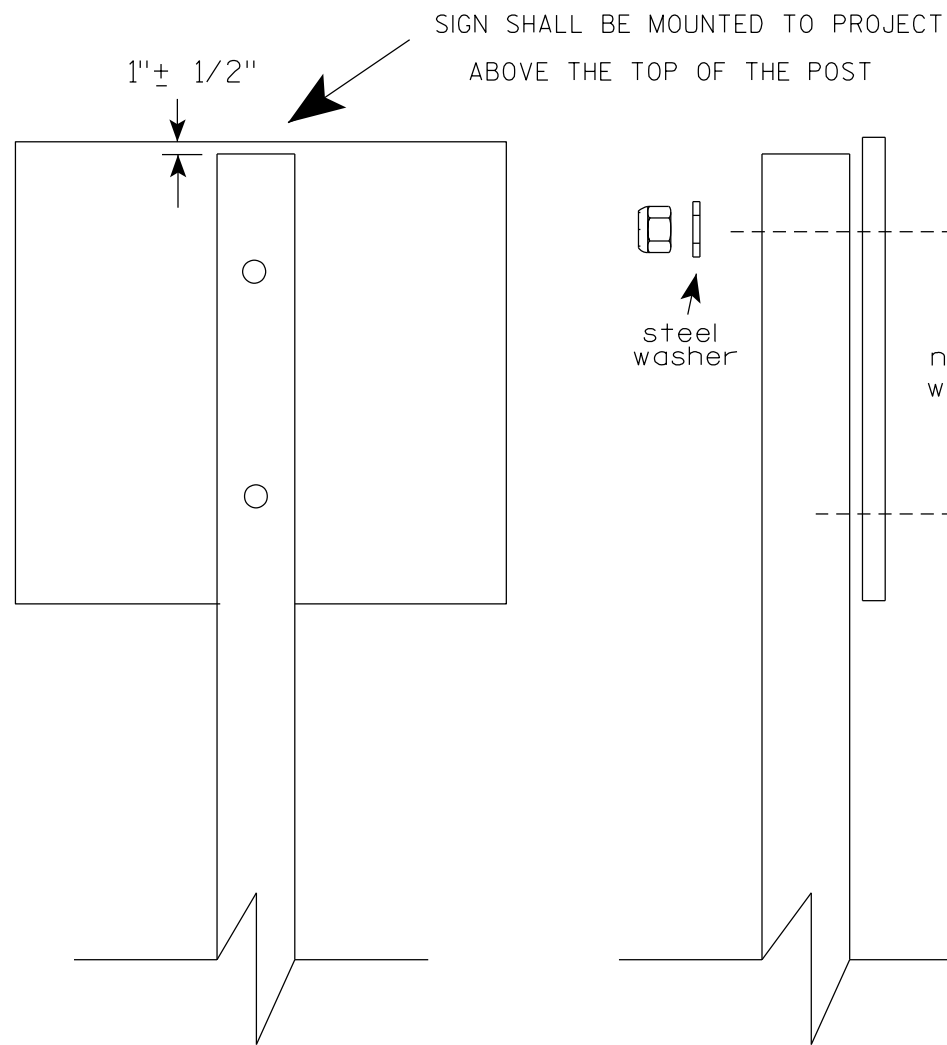
SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)	
L	E
Greater than 108" to 144"	12"

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS

WISCONSIN DEPT OF TRANSPORTATION
 APPROVED *Matthew R. Rauch*
 For State Traffic Engineer
 DATE 8/21/17 PLATE NO. A4-4.15



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

- Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
- Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

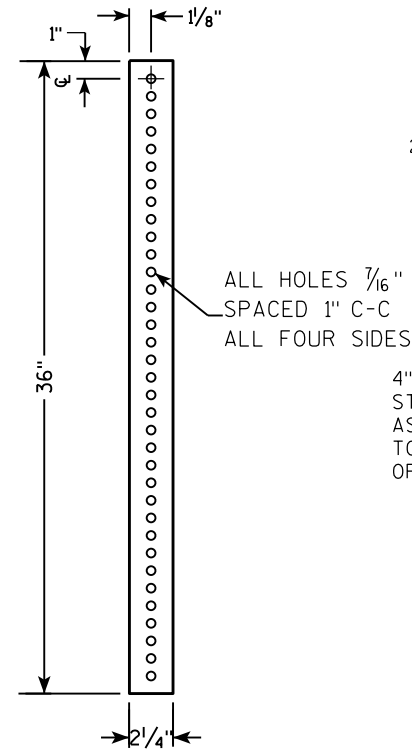
- STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)
- MACHINE BOLTS - $\frac{5}{16}$ " X 1-3/4" Length w/ lock nuts
- WOOD POSTS (4" x 6")
- LAG SCREWS - $\frac{3}{8}$ " X 3" (NO STRINGERS ON BACK OF SIGN)
 - $\frac{3}{8}$ " X 4" (STRINGERS ON BACK OF SIGN)
- SQUARE STEEL POSTS (2" x 2")
- MACHINE BOLTS - $\frac{3}{8}$ " X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)
 - $\frac{3}{8}$ " X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)
- RIVETS - $\frac{9}{32}$ " (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
- O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH
- WASHERS (ALL POSTS) -
- 1-1/4" O.D. X $\frac{3}{8}$ " I.D. X $\frac{1}{16}$ " STEEL
 - 1-1/4" O.D. X $\frac{3}{8}$ " I.D. X .080 NYLON

* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS TO POSTS	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R Rauch</i> For State Traffic Engineer
DATE 4/1/2020	PLATE NO. A4-8.9

**TELESCOPIC TUBING ANCHORS
TWO PIECE SYSTEM**

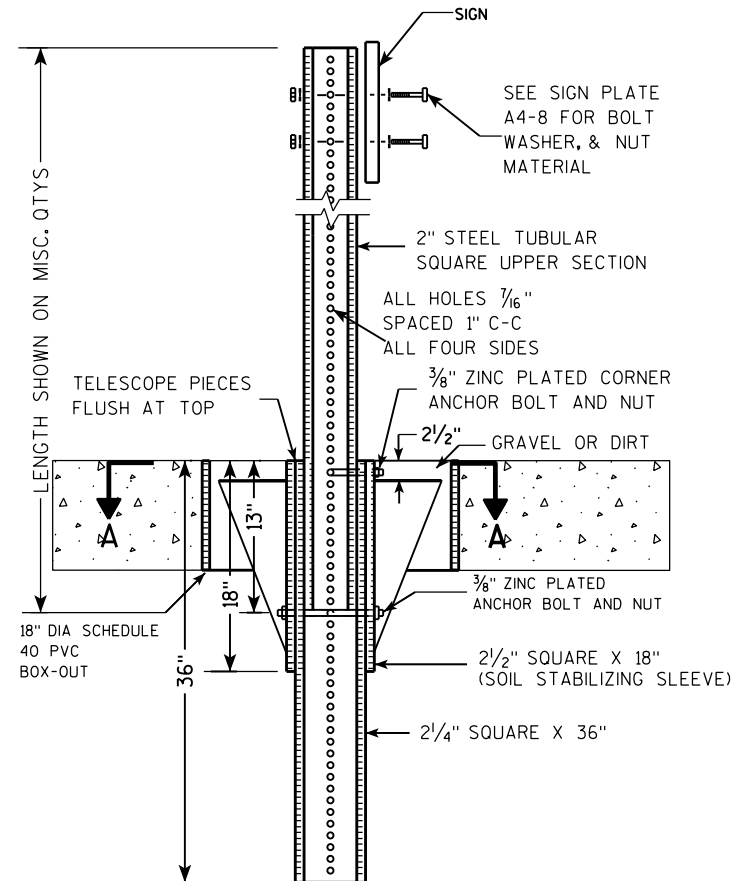
2 1/4" SQUARE
12 GAUGE
PERFORATED
GALVANIZED FINISH



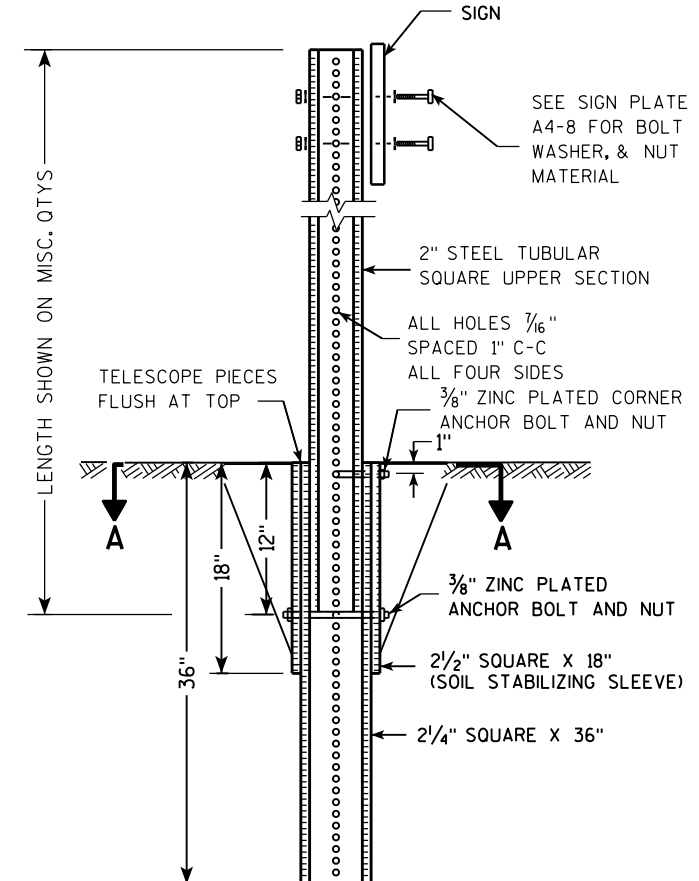
2 1/2" SQUARE
12 GAUGE
OMNI-DIRECTIONAL
PERFORATED
SOIL STABILIZING SLEEVE
GALVANIZED FINISH



**DETAIL OF TUBULAR STEEL SIGN POST
(IN POURED CONCRETE OR ASPHALT)**



**DETAIL OF TUBULAR STEEL SIGN POST
(IN LOCATIONS OTHER THAN POURED CONCRETE OR ASPHALT)**



Area of Sign Installation (Sq. Ft.)	Number of Required Posts
9 or less	1
Greater than 9 less than or equal to 18	2
Greater than 18 less than or equal to 27	3

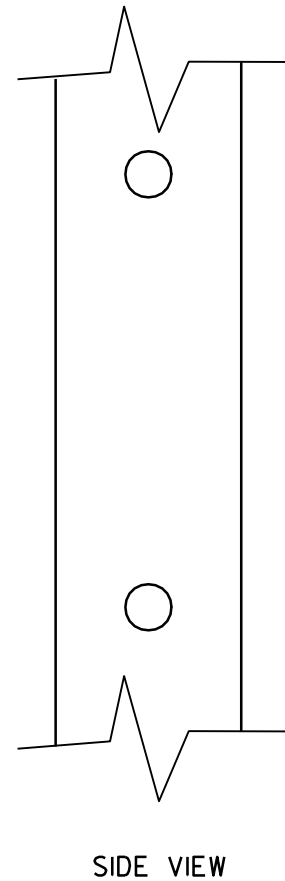
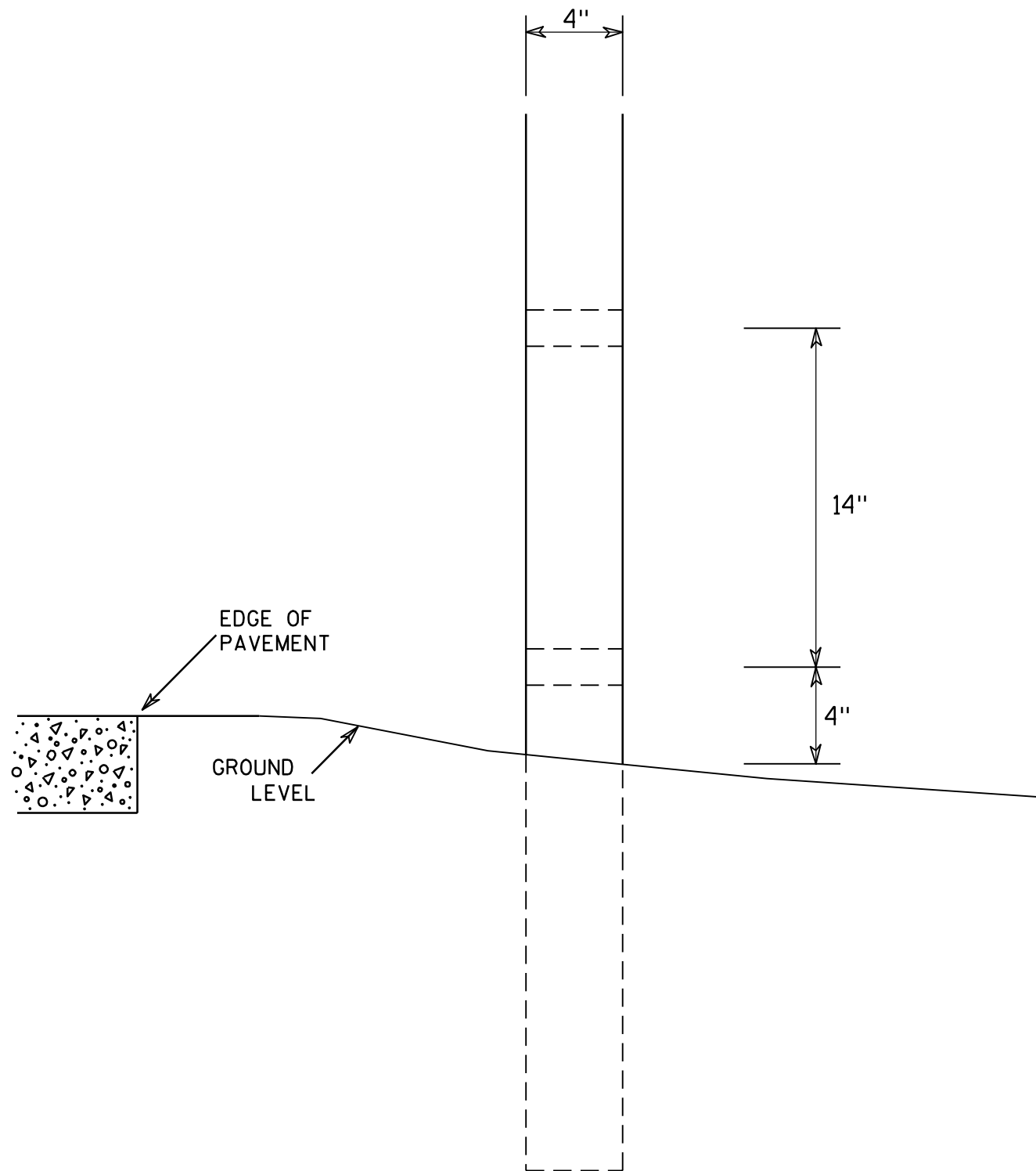
Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

**TUBULAR STEEL
SIGN POST
A4-9**

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 2/05/15 PLATE NO. A4-9.9



GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

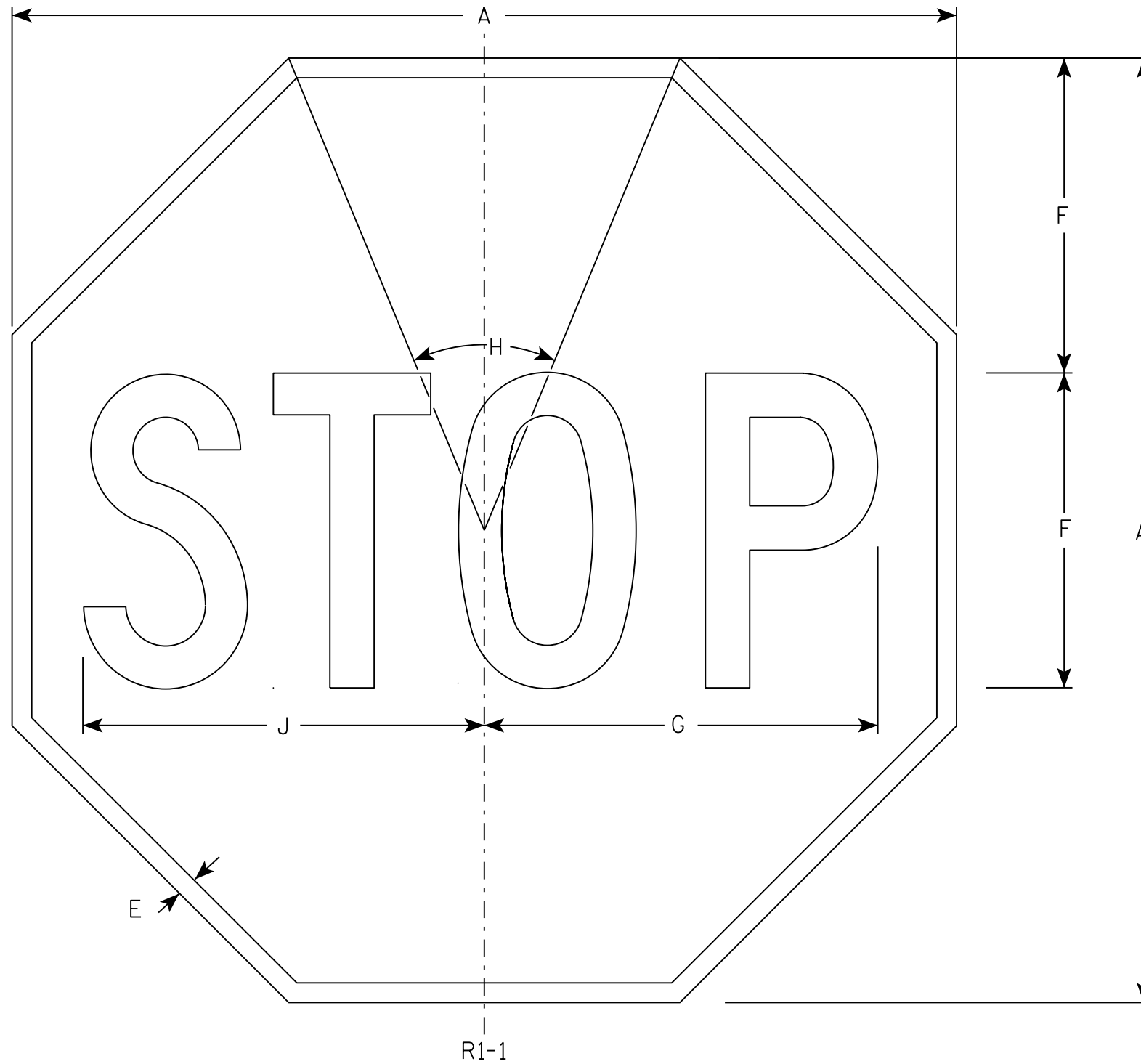
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4 X 6 WOOD POST MODIFICATIONS	
<i>WISCONSIN DEPT OF TRANSPORTATION</i>	
APPROVED	<i>Chester J. Spang</i> for State Traffic Engineer
DATE <u>3/27/97</u>	PLATE NO. <u>A4-11.2</u>

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Red
Message - White
3. Message Series - C



7

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SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	30				5/8	10	12 1/2	45°		12 3/4																	5.18
2S	30				5/8	10	12 1/2	45°		12 3/4																	5.18
2M	36				3/4	12	15	45°		15 3/8																	7.46
3	36				3/4	12	15	45°		15 3/8																	7.46
4	48				1	16	20	45°		20 1/2																	13.25
5	48				1	16	20	45°		20 1/2																	13.25
6	18				3/8	6	7 3/4	45°		7 3/4																	1.86
7	12				1/4	4	5	45°		5 1/8																	0.78

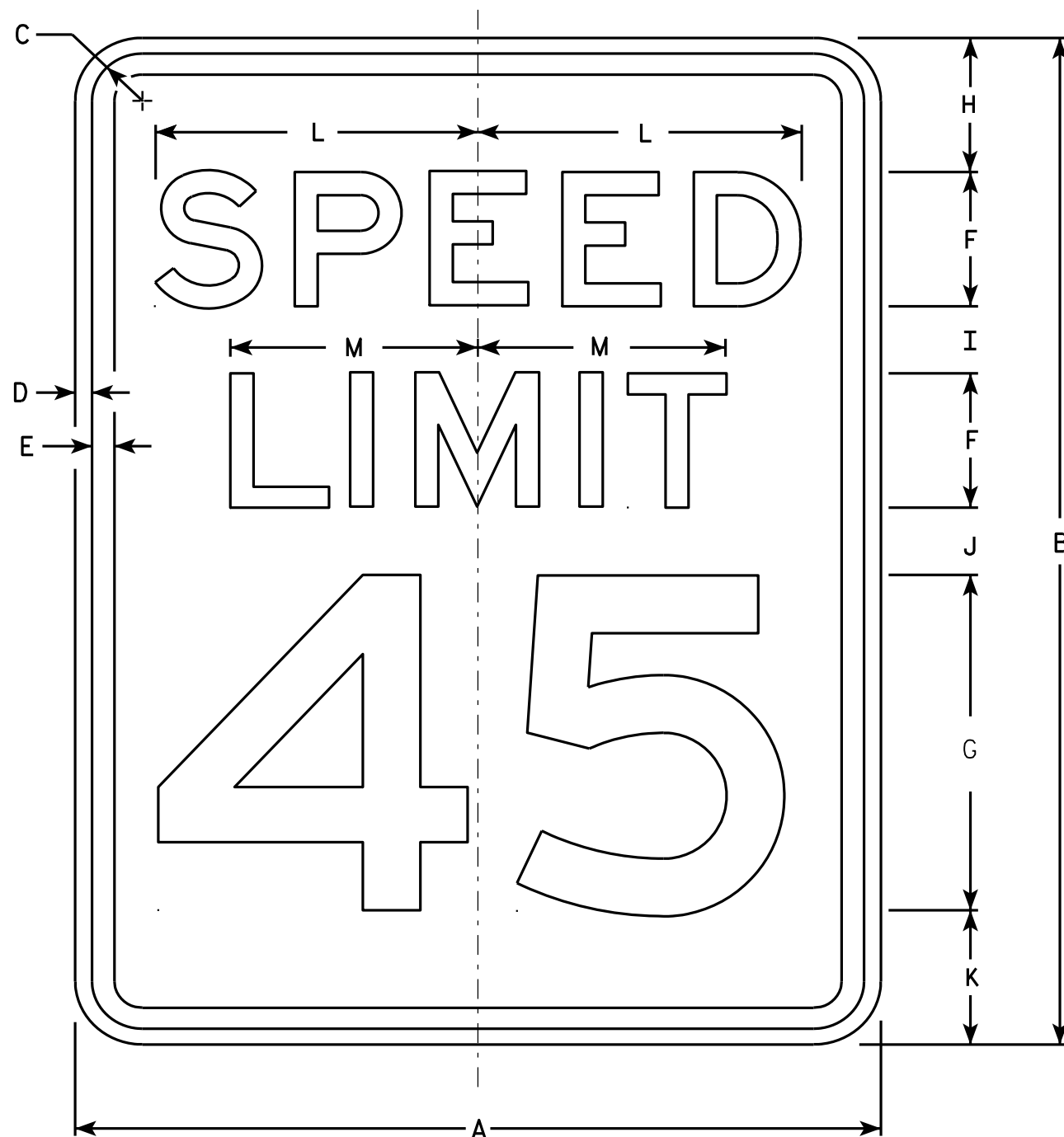
STANDARD SIGN
R1-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 11/12/15 PLATE NO. R1-1.13

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ **E**



R2-1

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - Black
3. Message Series - E
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Substitute appropriate numerals and optically adjust spacing to achieve proper balance.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	18	24	1 1/8	3/8	1/2	3	8	3	2	2	3	7 1/4	5 1/2														3.0
2S	24	30	1 1/8	3/8	1/2	4	10	3	2 1/4	3 3/8	3 3/8	9 5/8	7 3/8														5.0
2M	30	36	1 3/8	1/2	5/8	5	12	5	2 1/2	2 1/2	4	12	9 1/4														7.5
3	36	48	1 3/8	1/2	5/8	6	14	6	5	5	6	14 3/8	11														12.0
4	36	48	1 3/8	1/2	5/8	6	14	6	5	5	6	14 3/8	11														12.0
5	48	60	2 1/4	3/4	1	8	20	6	4 1/2	6 3/4	6 3/4	19 1/4	14 5/8														20.0

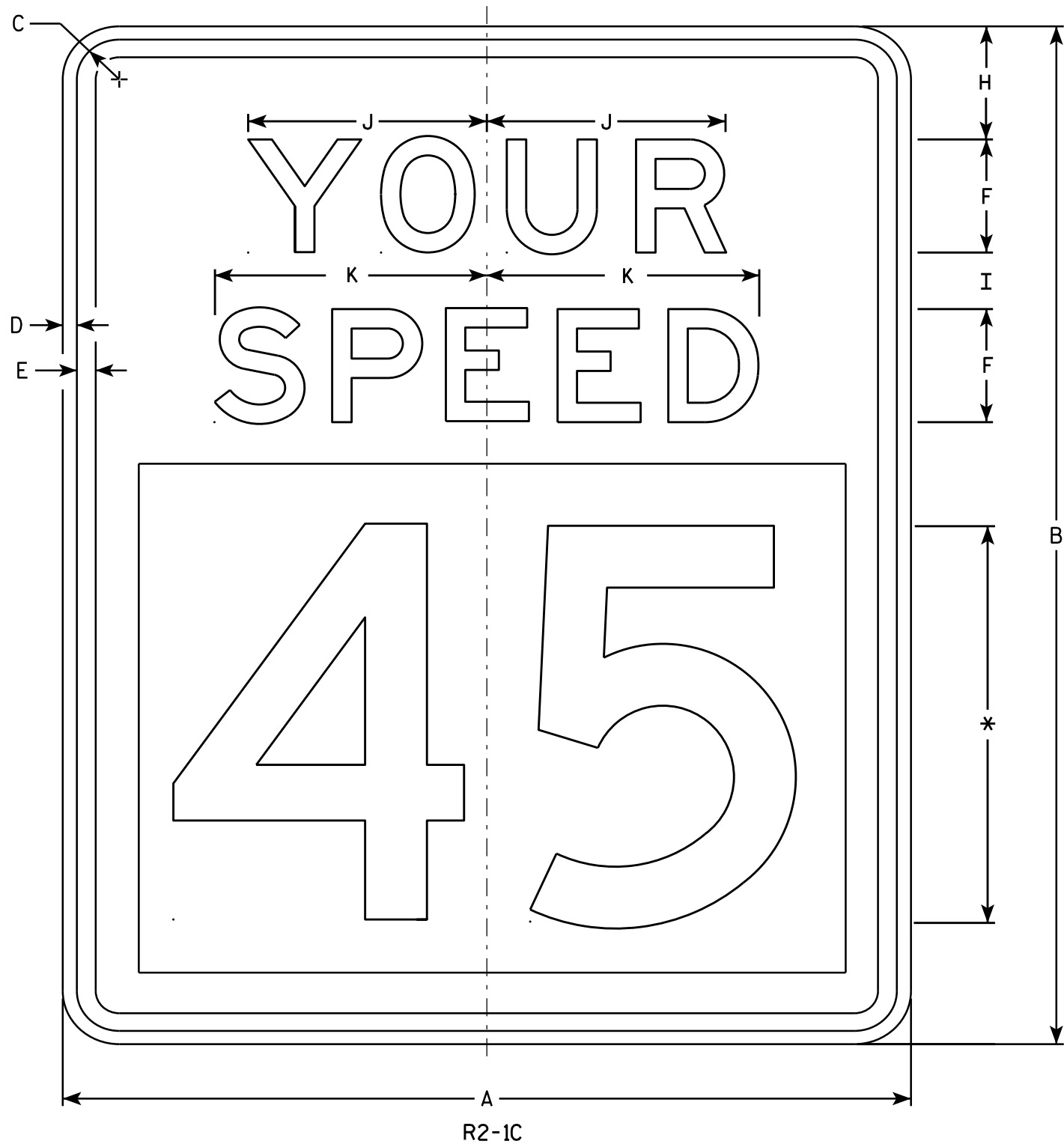
STANDARD SIGN
R2-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 5/26/10 PLATE NO. R2-1.13

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E



NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - E, Lines 1 & 2
4. LED screen shall have a minimum of 12" numbers for posted speeds of less than 45 MPH and a minimum of 18" numbers should be used for posted speeds of 45 MPH or greater.

* SEE NOTE 4

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	VARIES	VARIES	1 1/8	3/8	1/2	4		3	2 1/4	8 1/2	9 5/8																VARIES
2M	VARIES	VARIES	1 1/8	3/8	1/2	4		3	2 1/4	8 1/2	9 5/8																VARIES
3	VARIES	VARIES	1 3/8	1/2	5/8	6		6	5	13	15 1/2																VARIES
4	VARIES	VARIES	1 3/8	1/2	5/8	6		6	5	13	15 1/2																VARIES
5																											

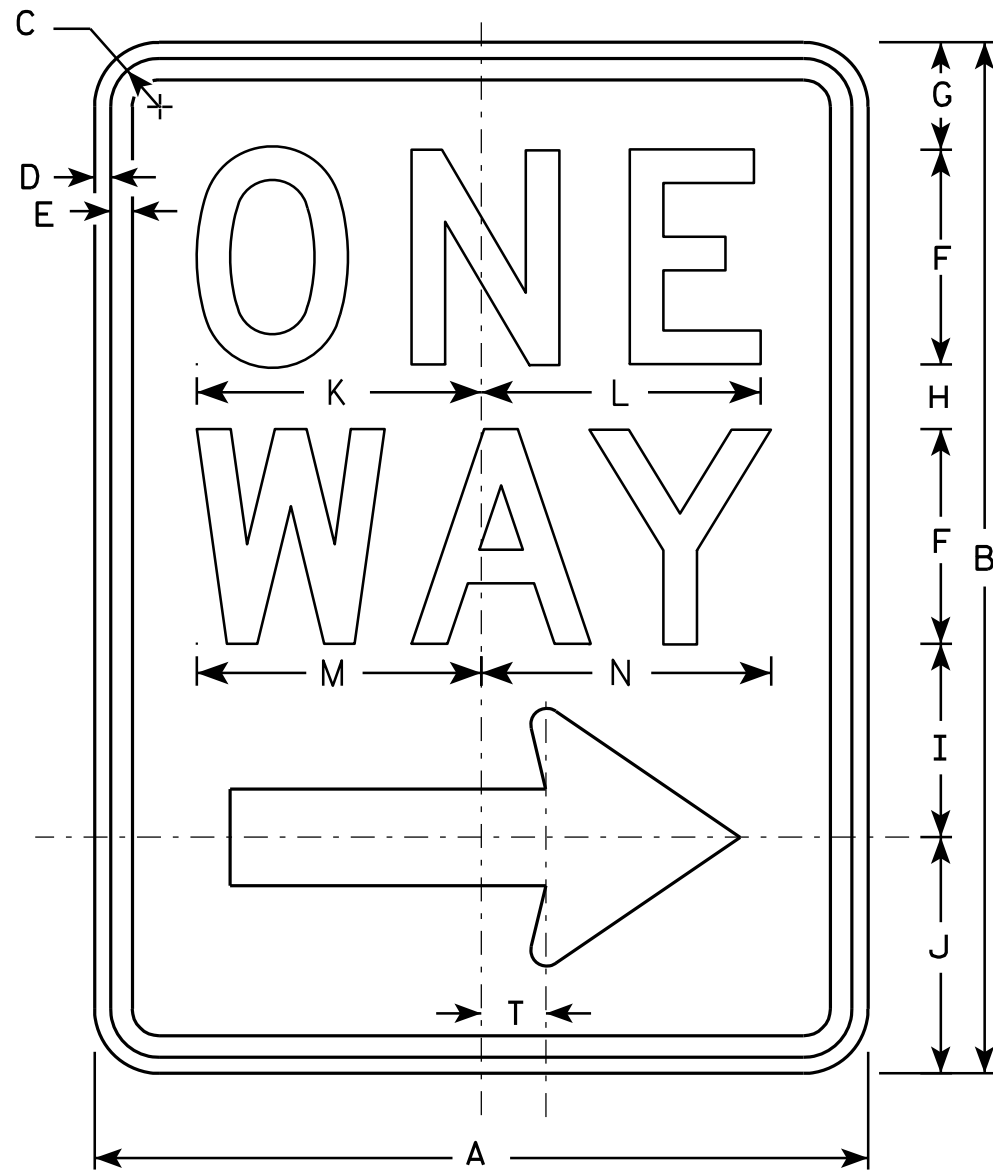
STANDARD SIGN
R2-1C

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 2/13/14 PLATE NO. R2-1C.3

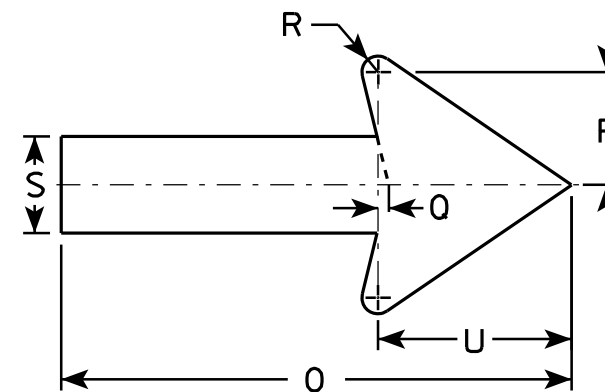
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**



R6-2R

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. R6-2L same as R6-2R except arrow points to the left.



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z
1	18	24	1 1/8	3/8	1/2	5	2 1/2	1 1/2	4 1/2	5 1/2	6 5/8	6 1/2	6 5/8	6 3/4	11 7/8	2 5/8	1/4	3/8	2 1/4	1 1/2	4 1/2					
2S	24	30	1 1/8	3/8	1/2	6	3	2 1/2	5 1/2	7	8 1/8	8 1/8	8 1/2	8 5/8	16	3 1/2	3/8	1/2	3	2	6					
2M	30	36	1 3/8	1/2	5/8	8	2 1/2	2 5/8	6 7/8	8	10 1/2	10 1/2	11 1/4	11 1/4	20	4 3/8	1/2	5/8	3 3/4	2 1/2	7 1/2					
3	36	48	1 7/8	1/2	5/8	10	5 1/4	3 1/4	9	10 1/2	12 3/4	12 3/4	13 1/4	13 1/2	24	5 5/8	1/2	3/4	4 3/4	3	9					
4	36	48	1 7/8	1/2	5/8	10	5 1/4	3 1/4	9	10 1/2	12 3/4	12 3/4	13 1/4	13 1/2	24	5 5/8	1/2	3/4	4 3/4	3	9					
5																										

STANDARD SIGN
R6-2 R&L

WISCONSIN DEPT OF TRANSPORTATION

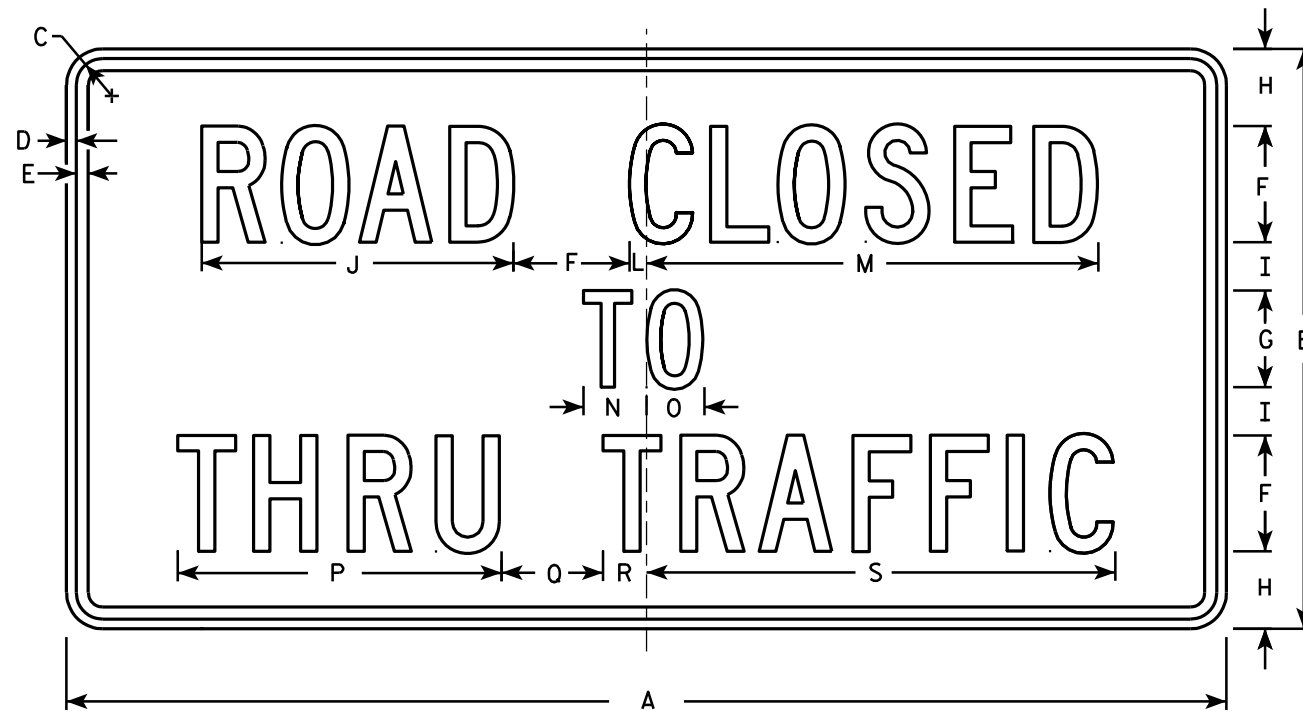
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 11/2/10 PLATE NO. R6-2.8

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



R11-4

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	60	30	1 3/8	1/2	5/8	6	5	4	2 1/2	16 1/8		7/8	23 3/8	3 1/4	3	16 3/4	5 1/4	2 1/4	24 1/4								12.5
2M	60	30	1 3/8	1/2	5/8	6	5	4	2 1/2	16 1/8		7/8	23 3/8	3 1/4	3	16 3/4	5 1/4	2 1/4	24 1/4								12.5
3																											
4																											
5																											

STANDARD SIGN
R11 - 4

WISCONSIN DEPT OF TRANSPORTATION

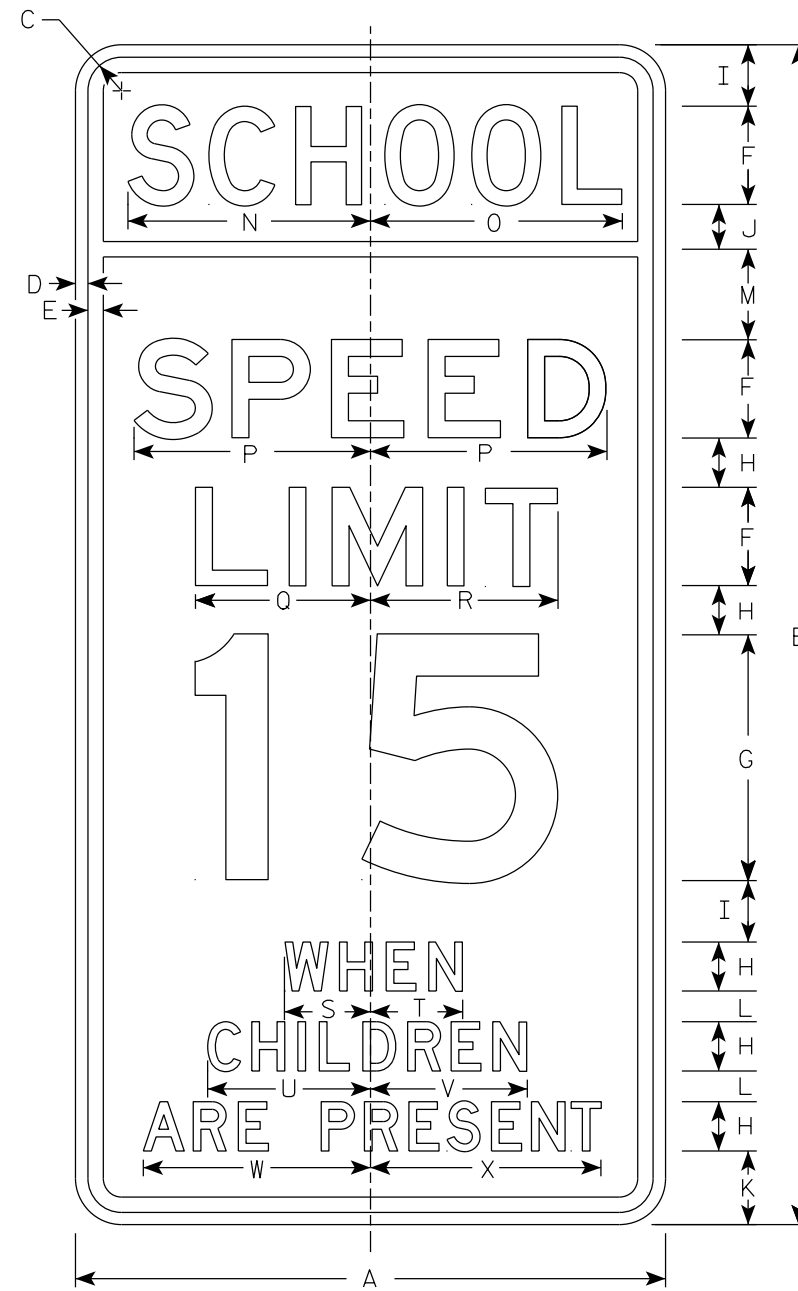
APPROVED *Matthew R. Raush*
for State Traffic Engineer

DATE 4/1/11 PLATE NO. R11-4.3

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E

NOTES

1. Sign is Type II - See Note 2 for Sheeting Type
2. Color:
Background - See Note 4
Message - Black
3. Message Series - See Note 5
4. Top panel (SCHOOL) background - Yellow Green - Type F Reflective
Lower panel background - White - Type SH Reflective
5. From top to bottom:
Lines 1, 5, 6 & 7 are series D
Lines 2, 3 & 4 are series E
6. Line 4 substitute appropriate numerals and adjust spacing to achieve proper balance.



S4-51

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	48	1 3/8	1/2	5/8	4	10	2	2 1/2	1 3/4	3	1 1/4	3 3/4	9 7/8	10 1/4	9 5/8	7 1/8	7 5/8	3 1/2	3 3/8	6 5/8	6 3/8	9 1/4	9 3/8			8.00
3	36	72	2 1/4	3/4	1	6	15	3	3 3/4	2 3/4	4 1/2	1 7/8	5 1/2	15	15 1/4	14 1/2	11 1/4	11 1/2	5 1/2	5 3/4	10	9 3/4	14	14 1/8			18.00
4																											
5																											

STANDARD SIGN
S4-51

WISCONSIN DEPT OF TRANSPORTATION

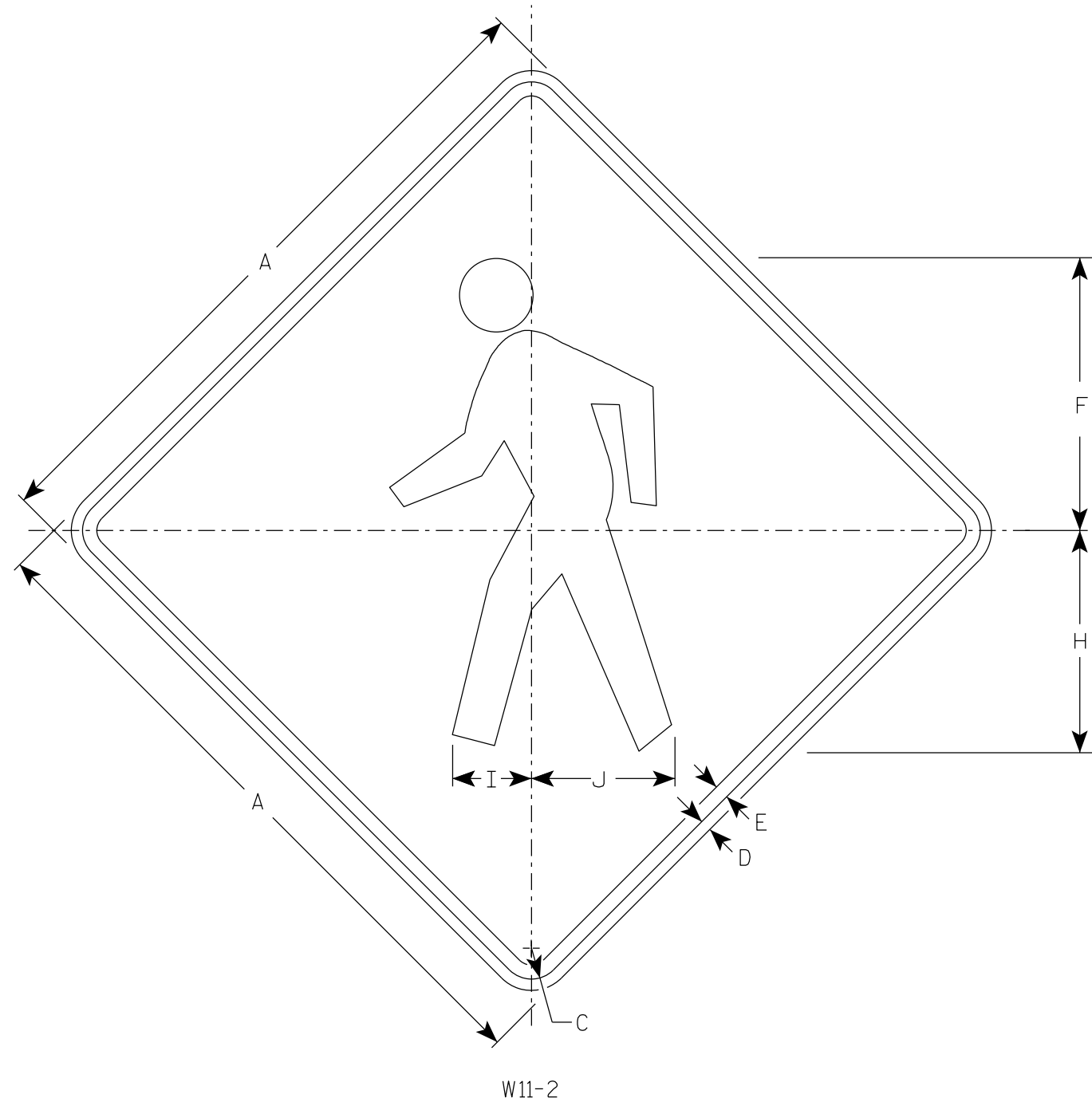
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 7/27/2020 PLATE NO. S4-51.10

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ **E**

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
 Background - Yellow
 Message - Black



W11-2

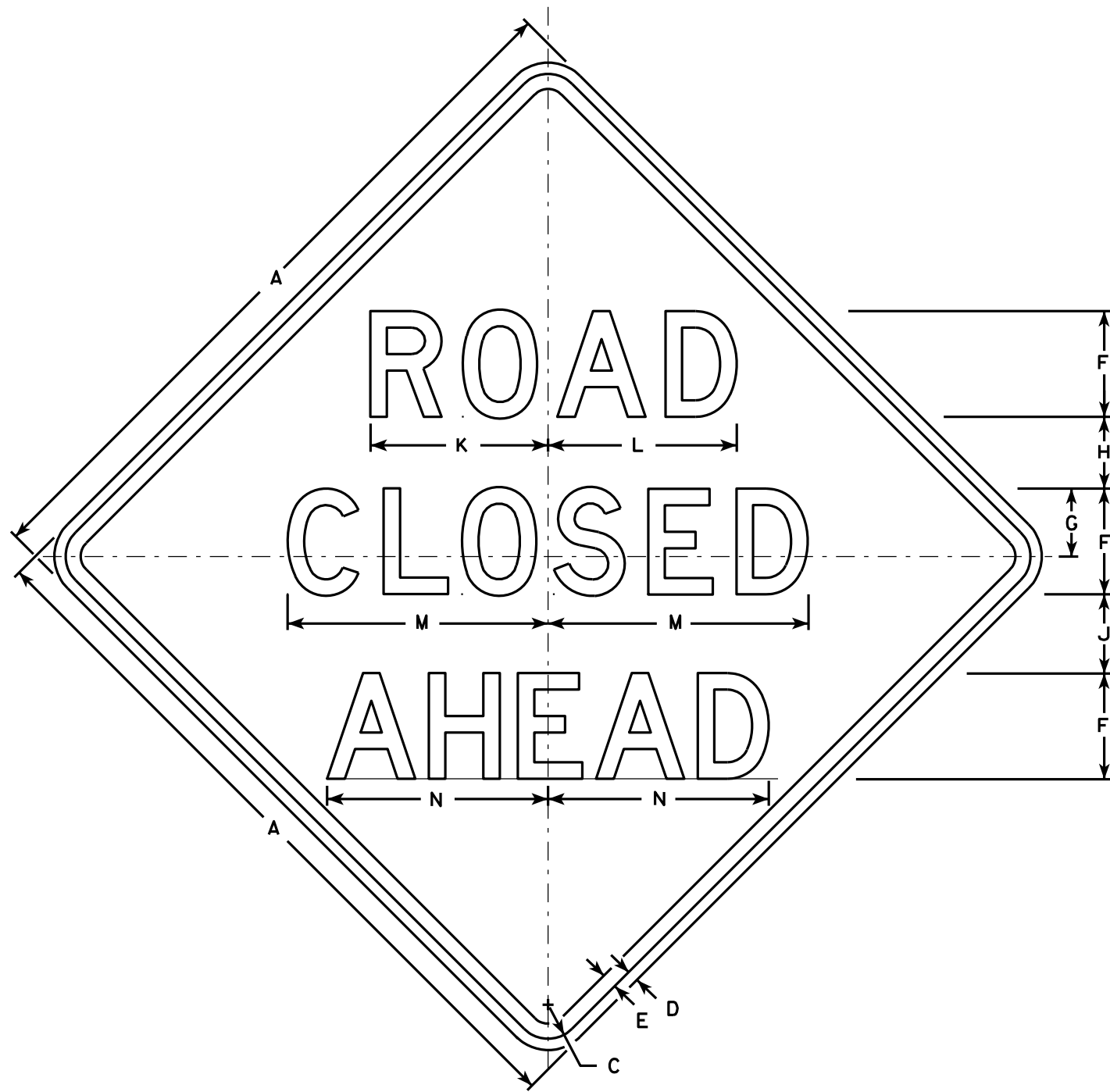
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2	9 3/4		7 7/8	2 7/8	5 1/8																	4.0
2S	30		1 3/8	1/2	5/8	12 1/8		9 7/8	3 1/2	6 3/8																	6.25
2M	36		1 5/8	5/8	3/4	14 1/2		11 7/8	4 1/4	7 5/8																	9.0
3	36		1 5/8	5/8	3/4	14 1/2		11 7/8	4 1/4	7 5/8																	9.0
4	48		2 1/4	3/4	1	19 3/8		15 3/4	5 5/8	10 1/4																	16.0
5																											

STANDARD SIGN
W11-2

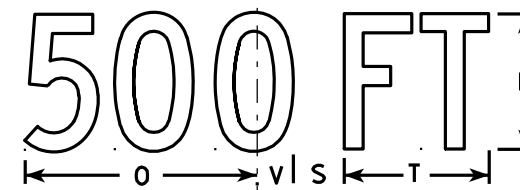
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Rauch*
for State Traffic Engineer

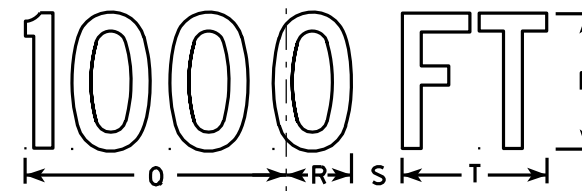
DATE 4/8/2020 PLATE NO. W11-2.8



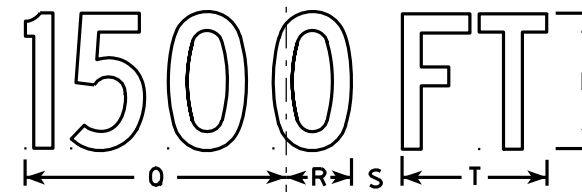
W20-3A



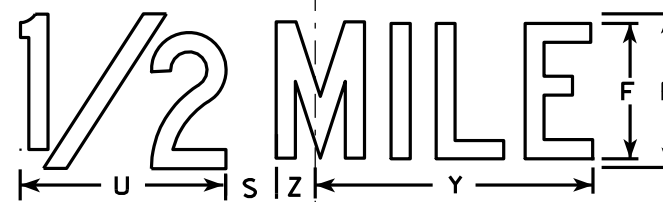
W20-3D



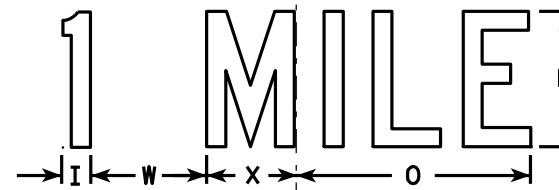
W20-3C



W20-3B



W20-3G



W20-3F

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - see note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Lines 1 and 2 are Series D.
Line 3 is Series D for AHEAD and Series C for all other distances.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	5	3 3/8	3 1/2	1 1/8	4	8 3/8	8 7/8	12 1/2	11	9	6	10 1/8	2 1/2	1 7/8	5 5/8	8	1 3/8	4 1/2	3 1/2	10 3/4	1 3/4	9.0
2S	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
2M	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
3	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
4	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
5	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0

STANDARD SIGN
W20-3A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 3/18/11 PLATE NO. W20-3.7

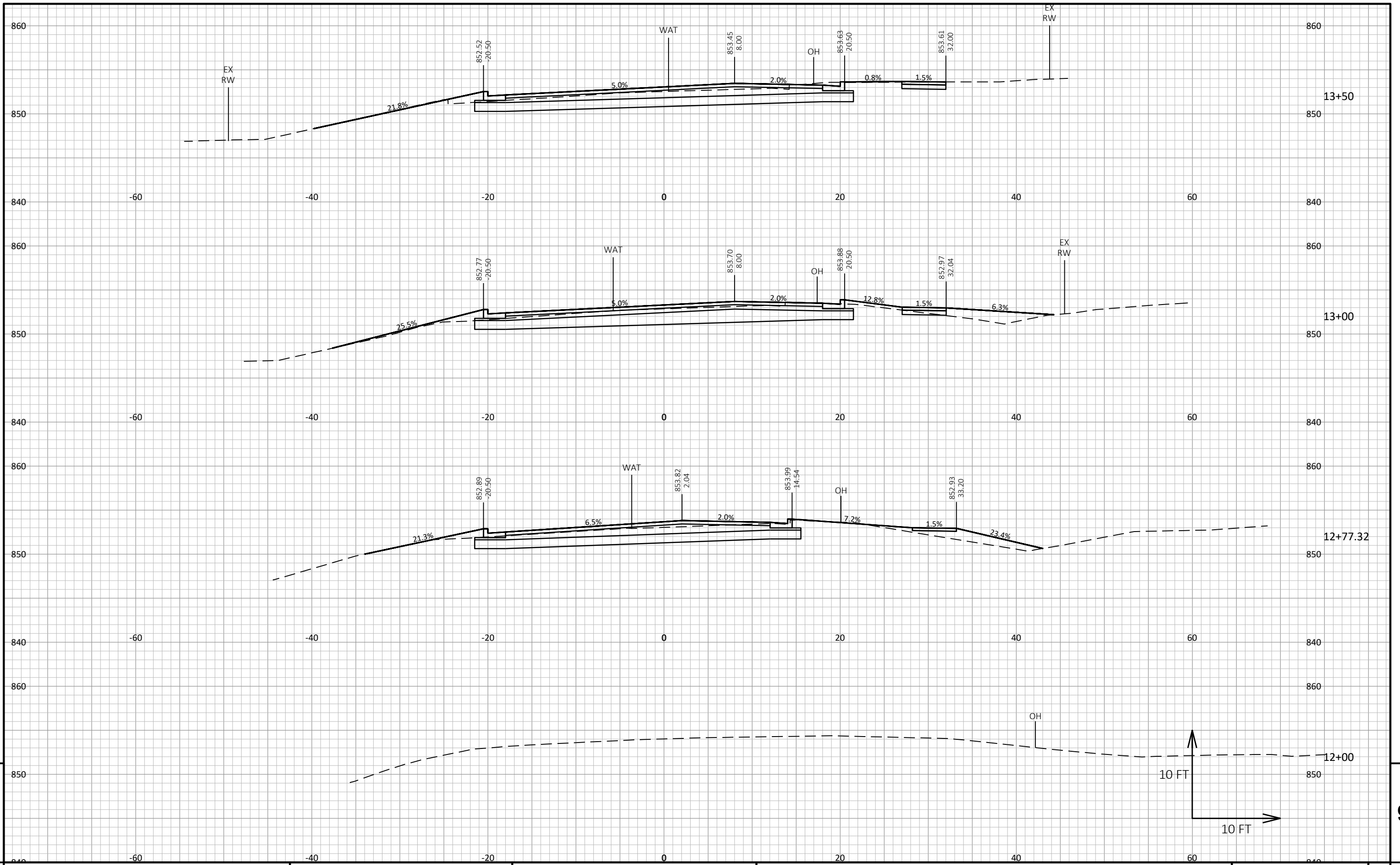
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E

EARTHWORK - MAINLINE

STATION	Distance	Area			Incremental Volume			Cumulative Vol (CY)			Mass Ordinate Note 6
		Cut (SF)	Fill (SF)	EBS (SF)	Cut Note 1 (CY)	Fill Note 2 (CY)	EBS Note 3 (CY)	Cut 1.00 Note 4	Fill 1.00 Note 5	EBS 1.00	
12+50		0	0	0	0	0	0	0	0	0	0
12+77	27	70	19	7	35	9	3	35	9	3	25
13+00	23	71	20	7	61	17	6	96	26	10	70
13+50	50	74	21	7	135	37	13	230	63	23	167
14+00	50	75	19	8	138	37	14	369	100	37	268
14+23	23	75	16	7	63	15	6	432	115	43	317
14+50	27	80	14	8	78	15	8	510	130	51	380
14+85	35	86	9	9	106	14	11	616	144	62	472
15+09	24	92	4	9	80	6	8	696	150	70	547
15+50	41	94	9	9	141	10	14	838	159	84	678
16+00	50	95	8	9	174	15	17	1012	174	101	837
16+47	47	102	3	10	171	10	17	1183	184	118	999
17+00	53	104	2	10	201	5	20	1384	189	138	1195
17+36	36	103	2	10	139	2	14	1523	191	152	1332
17+64	28	-4	1	0	51	1	5	1574	192	157	1382
17+81	17	-12	1	-1	-5	1	-1	1569	193	157	1376
18+42	61	101	4	10	101	6	10	1670	199	167	1471
18+94	51	102	5	10	193	9	19	1863	208	186	1655
19+26	33	89	7	9	115	7	12	1978	215	198	1763
19+50	24	94	8	9	81	7	8	2059	222	206	1837
19+91	41	87	15	9	136	18	14	2195	239	220	1956
20+63	73	92	12	9	240	37	24	2435	276	244	2159
21+00	37	95	9	9	128	14	13	2563	290	256	2273
21+48	48	-8	0	-1	78	8	8	2640	298	264	2342
22+09	61	96	7	10	100	8	10	2741	307	274	2434
22+58	49	94	7	9	171	13	17	2912	320	291	2592
22+85	27	91	11	9	92	9	9	3004	329	300	2675
23+00	15	91	15	9	51	7	5	3055	336	305	2719
23+40	40	92	15	9	135	22	14	3190	358	319	2832
24+00	60	77	10	8	189	27	19	3379	385	338	2993
24+50	50	-7	1	-1	65	10	6	3444	395	344	3049
25+00	50	98	7	10	84	7	8	3528	402	353	3126
25+50	50	82	4	8	167	10	17	3695	413	369	3282
26+05	55	96	9	10	181	13	18	3876	426	388	3450
26+50	45	94	6	9	159	12	16	4035	438	403	3597
26+72	22	96	5	10	78	5	8	4113	442	411	3671
27+00	28	96	7	10	98	6	10	4212	449	421	3763
27+31	31	97	8	10	111	9	11	4322	458	432	3865
27+50	19	99	4	10	69	4	7	4392	462	439	3930
28+00	50	101	5	10	185	8	18	4577	470	458	4106
28+62	62	102	1	10	233	7	23	4809	477	481	4332
28+82	20	102	2	10	74	1	7	4883	479	488	4404
29+03	22	98	3	10	80	2	8	4963	480	496	4482
29+50	47	92	10	9	164	11	16	5126	491	513	4635
29+78	28	90	10	9	94	10	9	5221	501	522	4719
30+00	22	89	10	9	72	8	7	5293	509	529	4784
30+50	50	91	11	9	166	19	17	5459	528	546	4931
30+66	16	93	9	9	55	6	5	5514	534	551	4980
30+97	31	98	9	10	110	10	11	5623	544	562	5079
31+19	22	104	5	10	83	6	8	5707	551	571	5156
31+50	31	98	7	10	115	7	11	5821	558	582	5264
32+00	50	100	2	10	183	8	18	6005	565	600	5439
32+41	41	99	0	10	152	2	15	6157	567	616	5590
33+08	67	79	20	8	219	25	22	6376	592	638	5784
33+50	42	77	23	8	121	34	12	6497	626	650	5871
33+89	39	85	14	8	118	27	12	6615	653	662	5962
34+13	24	90	10	9	77	10	8	6692	664	669	6028
34+63	49	101	5	10	174	13	17	6867	677	687	6190
35+00	37	94	10	9	135	10	14	7002	687	700	6315
35+40	40	94	10	9	138	14	14	7139	702	714	6438
36+05	65	116	0	12	253	12	25	7392	713	739	6679
36+50	45	126	1	13	203	1	20	7595	715	760	6881
37+00	50	131	1	13	238	2	24	7833	717	783	7116
37+29	29	134	0	13	139	0	14	7972	717	797	7255

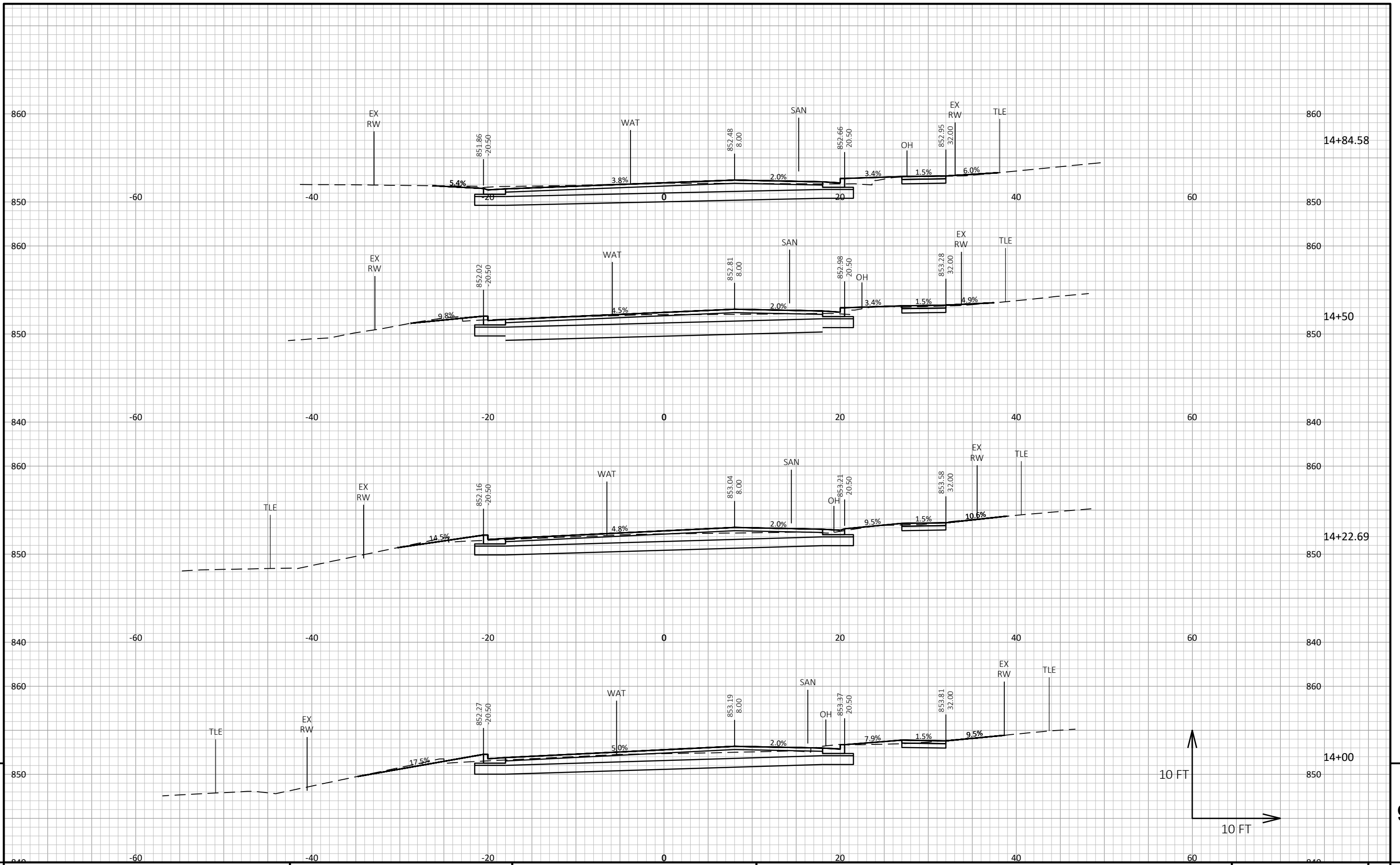
EARTHWORK - MAINLINE (CONTINUED)

STATION	Distance	Area			Incremental Volume			Cumulative Vol (CY)			Mass Ordinate Note 6
		Cut (SF)	Fill (SF)	EBS (SF)	Cut Note 1 (CY)	Fill Note 2 (CY)	EBS Note 3 (CY)	Cut 1.00 Note 4	Fill 1.00 Note 5	EBS 1.00	
37+50	21	128	2	13	104	1	10	8076	718	808	7358
38+00	50	1	4	0	119	5	12	8195	723	820	7472
38+28	28	106	0	11	55	2	5	8250	725	825	7525
38+57	29	94	8	9	108	4	11	8358	729	836	7628
39+00	43	91	11	9	148	15	15	8506	744	851	7762
39+12	12	86	12	9	40	5	4	8546	749	855	7797
39+50	38	86	13	9	120	17	12	8666	767	867	7900
40+08	58	103	1	10	203	15	20	8869	782	887	8087
40+18	11	102	2	10	40	1	4	8909	782	891	8126
40+50	32	100	4	10	118	3	12	9027	786	903	8241
40+77	27	103	3	10	101	3	10	9128	789	913	8339
41+03	27	106	0	11	102	1	10	9230	790	923	8440
41+50	47	96	3	10	173	3	17	9404	793	940	8611
42+00	50	90	5	9	172	7	17	9575	800	958	8775
42+50	50	93	7	9	169	10	17	9744	810	974	8934
42+61	11	92	7	9	39	3	4	9783	813	978	8970
43+00	39	87	11	9	127	13	13	9910	827	991	9084
43+50	50	90	11	9	163	20	16	10073	847	1007	9226
44+00	50	101	2	10	177	12	18	10250	859	1025	9392
44+25	25	99	2	10	93	2	9	10343	860	1034	9483
45+00	75	0	0	0	136	3	14	10480	864	1048	9616
45+58	58	96	7	10	104	7	10	10583	871	1058	9712
46+00	42	94	6	9	146	10	15	10729	881	1073	9849
46+26	26	94	6	9	89	6	9	10819	886	1082	9933
46+39	13	94	5	9	47	3	5	10866	889	1087	9977
47+00	61	90	10	9	208	17	21	11074	906	1107	10168
47+50	50	89	15	9	166	23	17	11240	929	1124	10312
47+69	19	91	12	9	62	9	6	11302	938	1130	10364
47+82	13	90	15	9	44	7	4	11346	945	1135	10402
48+08	26	92	8	9	88	11	9	11435	956	1143	10479
48+16	8	90	12	9	28	3	3	11463	959	1146	10504
48+50	34	90	15	9	112	16	11	11574	975	1157	10599
49+00	50	97	6	10	173	19	17	11748	995	1175	10753
49+19	19	94	13	9	66	7	7	11814	1001	1181	10813
49+37	19	102	5	10	68	6	7	11882	1007	1188	10875
49+50	13	101	6	10	47	2	5	11929	1010	1193	10919
49+83	33	99	4	10	123	6	12	12052	1016	1205	11036
50+00	17	96	7	10	61	3	6	12113	1019	1211	11094
50+17	17	98	7	10	62	4	6	12175	1023	1217	11151
50+60	43	93	10	9	150	13	15	12325	1036	1233	11289
50+90	31	92	8	9	104	10	10	12430	1046	1243	11384
51+04	13	90	8	9	44	4	4	12474	1050	1247	11424
51+38	34	89	7	9	113	10	11	12587	1060	1259	11527
51+50	12	84	10	8	39	4	4	12626	1064	1263	11562
52+00	50	92	6	9	164	14	16	12790	1078	1279	11711
52+27	27	100	2	10	96	4	10	12885	1082	1289	11803
52+64	37	106	0	11	139	1	14	13025	1084	1302	11941
52+86	22	100	1	10	84	0	8	13109	1084	1311	12025
53+00	14	101	2	10	53	1	5	13162	1085	1316	12078
53+28	28	98	7	10	103	5	10	13265	1089	1327	12176
53+68	40	97	4	10	142	8	14	13407	1098	1341	12310
53+90	23	100	12	10	83	7	8	13490	1104	1349	12386
54+00	10	102	12	10	36	4	4	13527	1109	1353	12418
54+39	39	97	7	10	144	13	14	13670	1122	1367	12548
54+50	11	96	7	10	39	3	4	13709	1125	1371	12584
54+74	24	95	8	9	84	7	8	13794	1132	1379	12662
55+00	26	87	14	9	88	11	9	13882	1143	1388	12739
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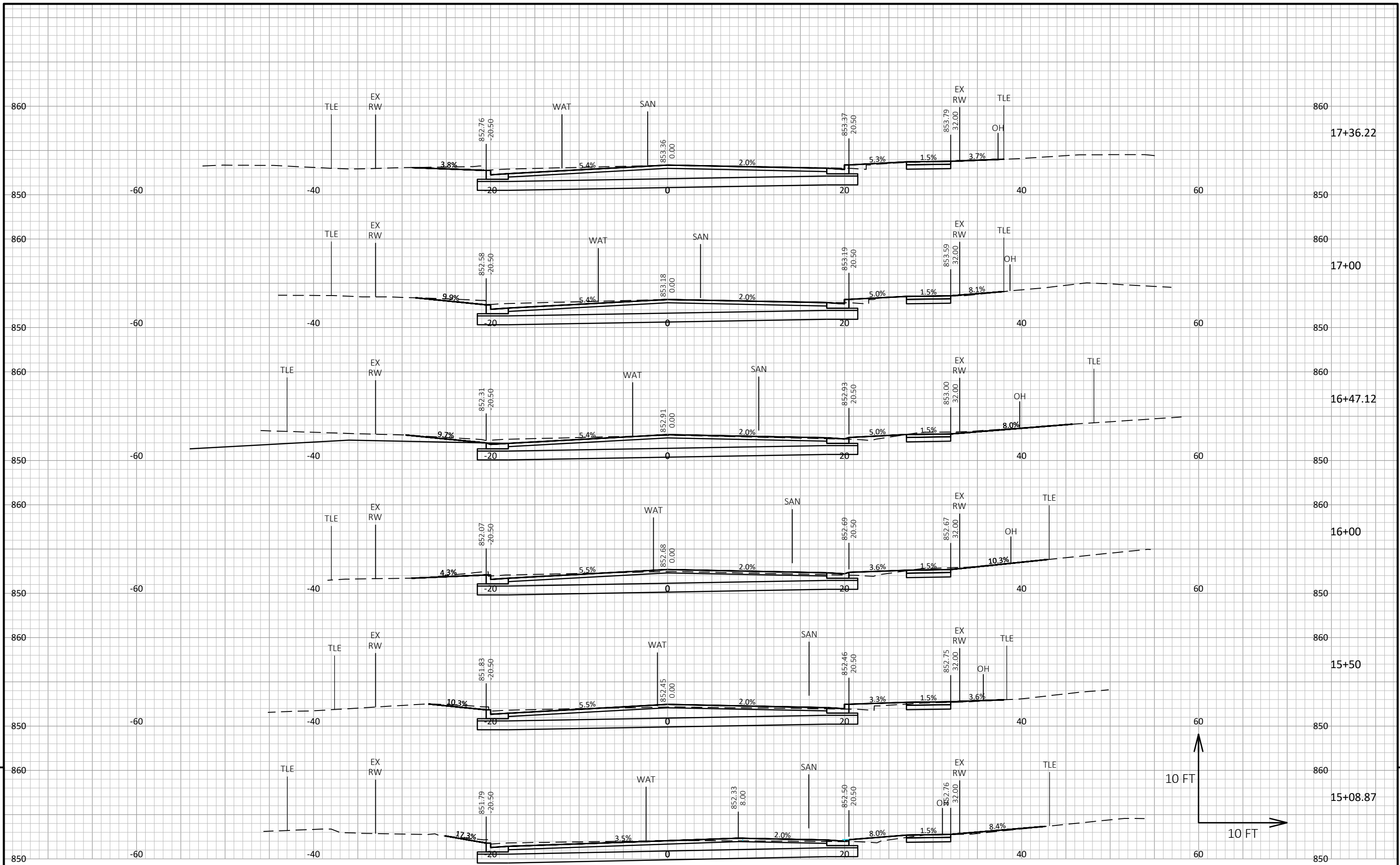


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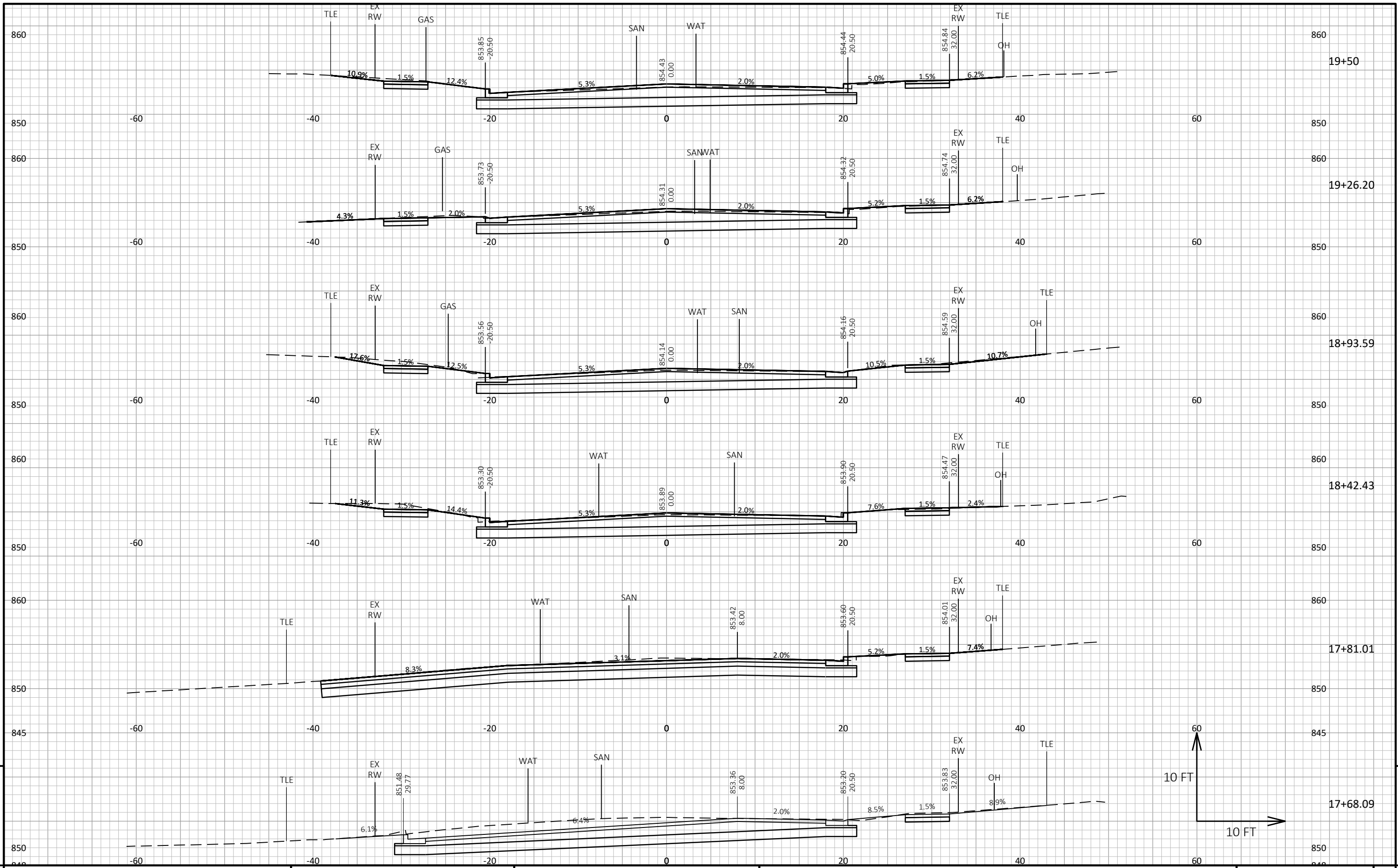
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PROJECT NO: 5685-00-05 HWY: EXCHANGE STREET COUNTY: DANE CROSS SECTIONS: SECTION VIEW GROUP - 1 SHEET 101 E



PROJECT NO: 5685-00-05 HWY: EXCHANGE STREET COUNTY: DANE CROSS SECTIONS: SECTION VIEW GROUP - 1 SHEET 102

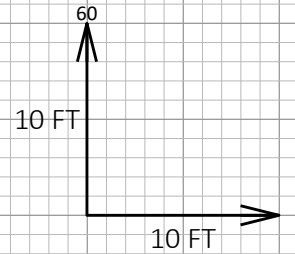


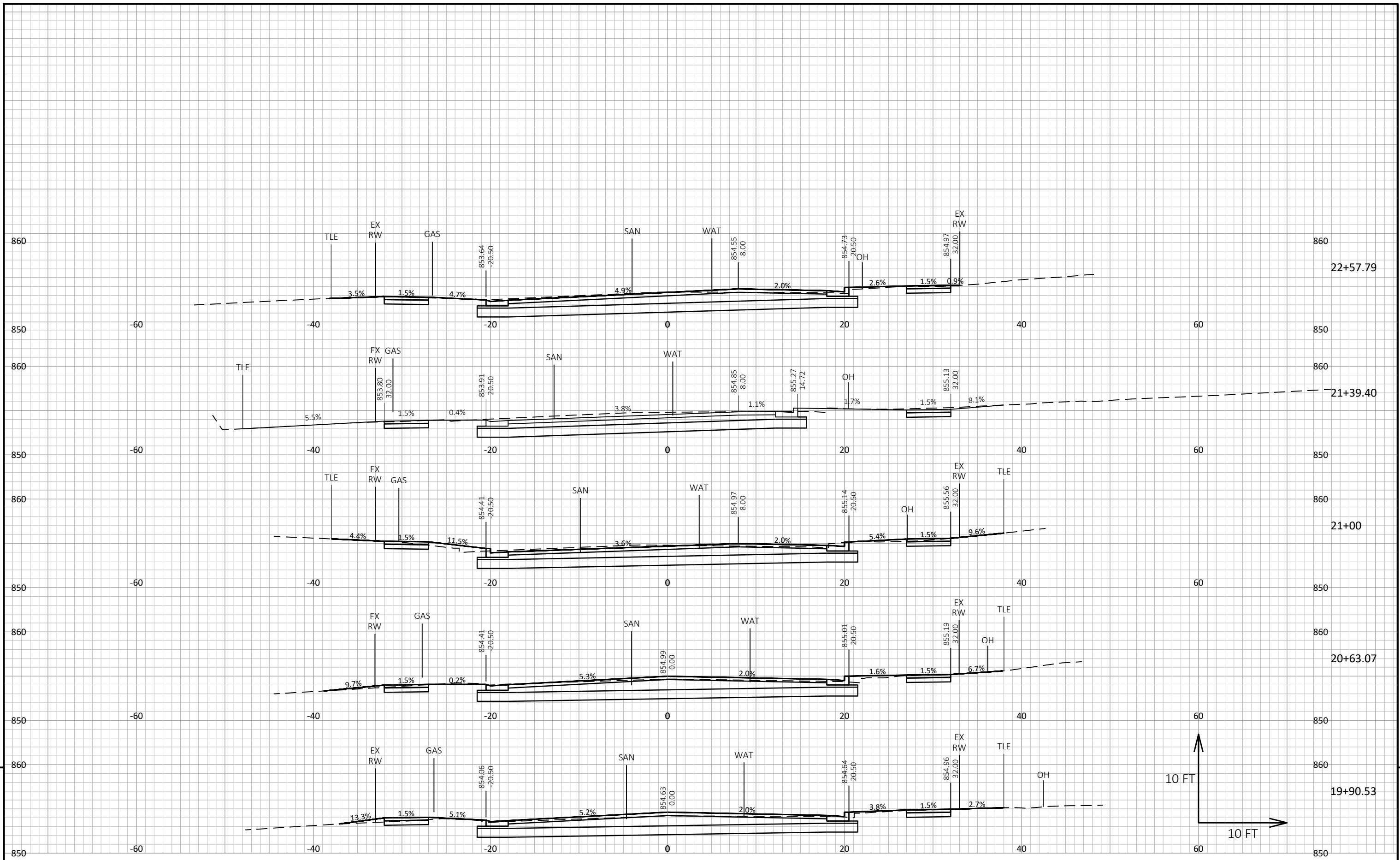
PROJECT NO: 5685-00-05 HWY: EXCHANGE STREET COUNTY: DANE CROSS SECTIONS: SECTION VIEW GROUP - 1 SHEET 103

FILE NAME: J:\JOB#5\MCFARLAND\MC-134-M4 EXCHANGE STREET PRELIMINARY ENGINEERING (FILE)\DRAWING\MC 134 ALIGNMENT CORRIDOR.DWG PLOT DATE: 7/31/2023 5:03 PM PLOT BY: BEN PINGEL PLOT NAME: PLOT SCALE: 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49

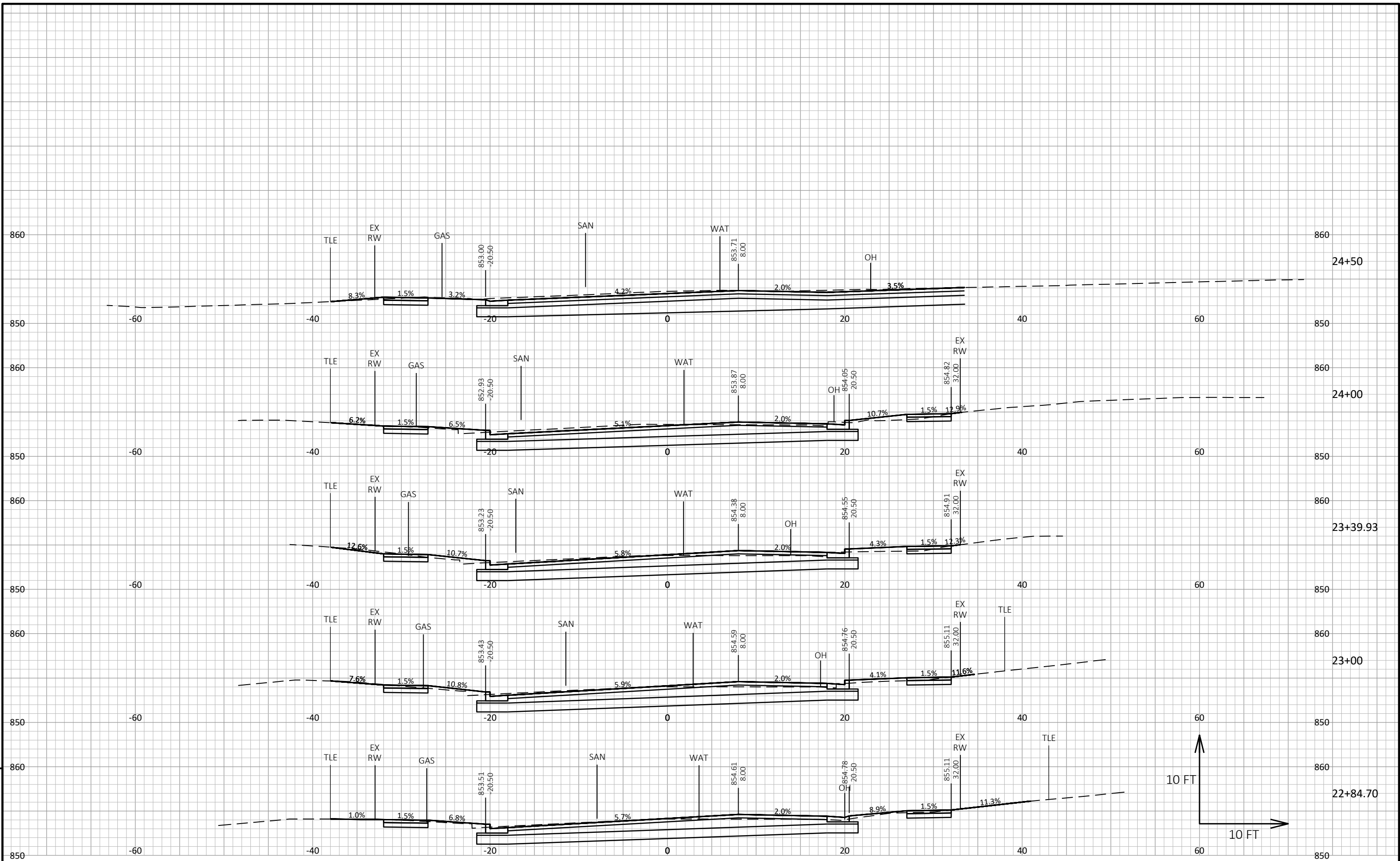
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PROJECT NO: 5685-00-05 HWY: EXCHANGE STREET COUNTY: DANE CROSS SECTIONS: SECTION VIEW GROUP - 1 SHEET 104



PROJECT NO: 5685-00-05

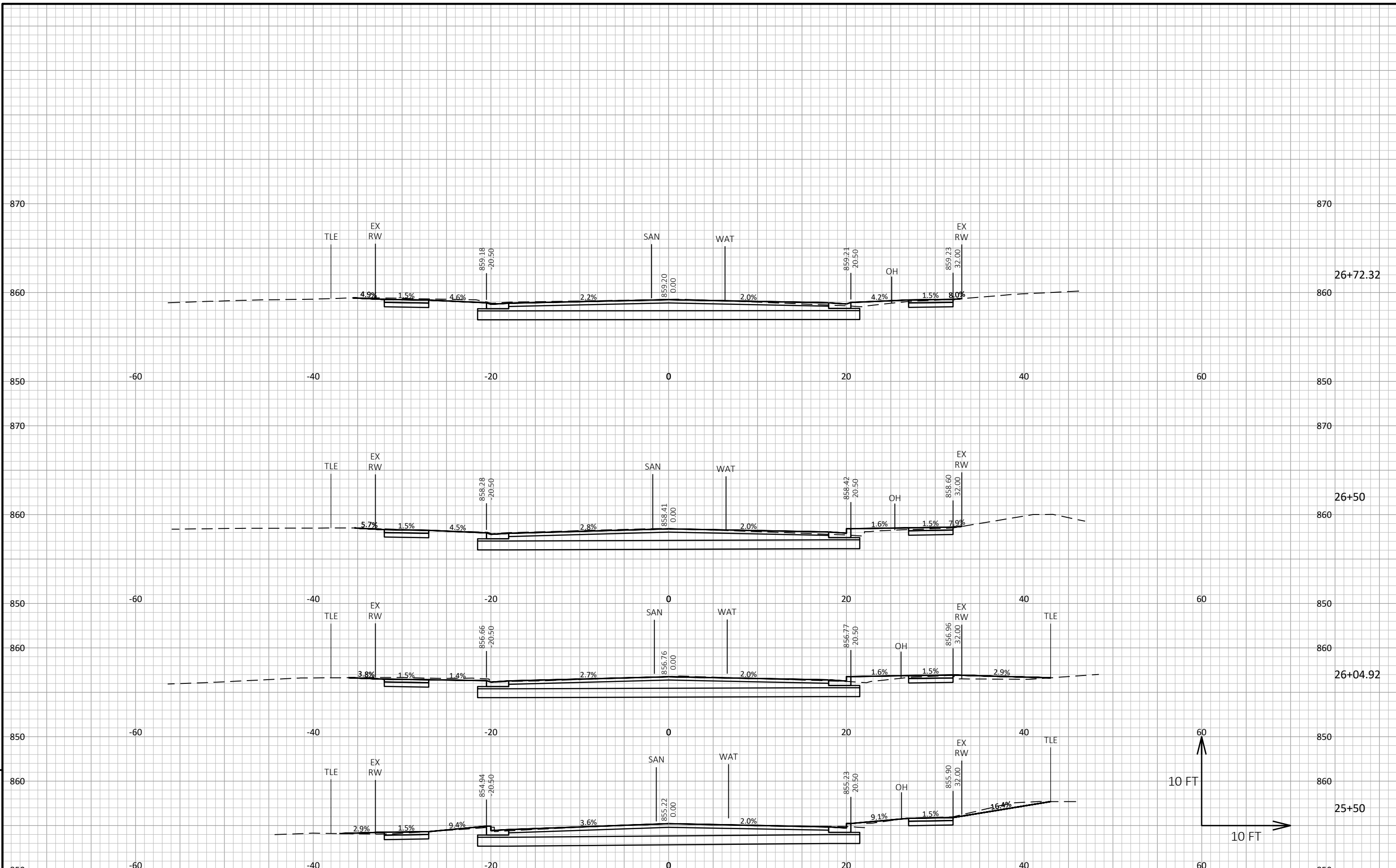
HWY: EXCHANGE STREET

COUNTY: DANE

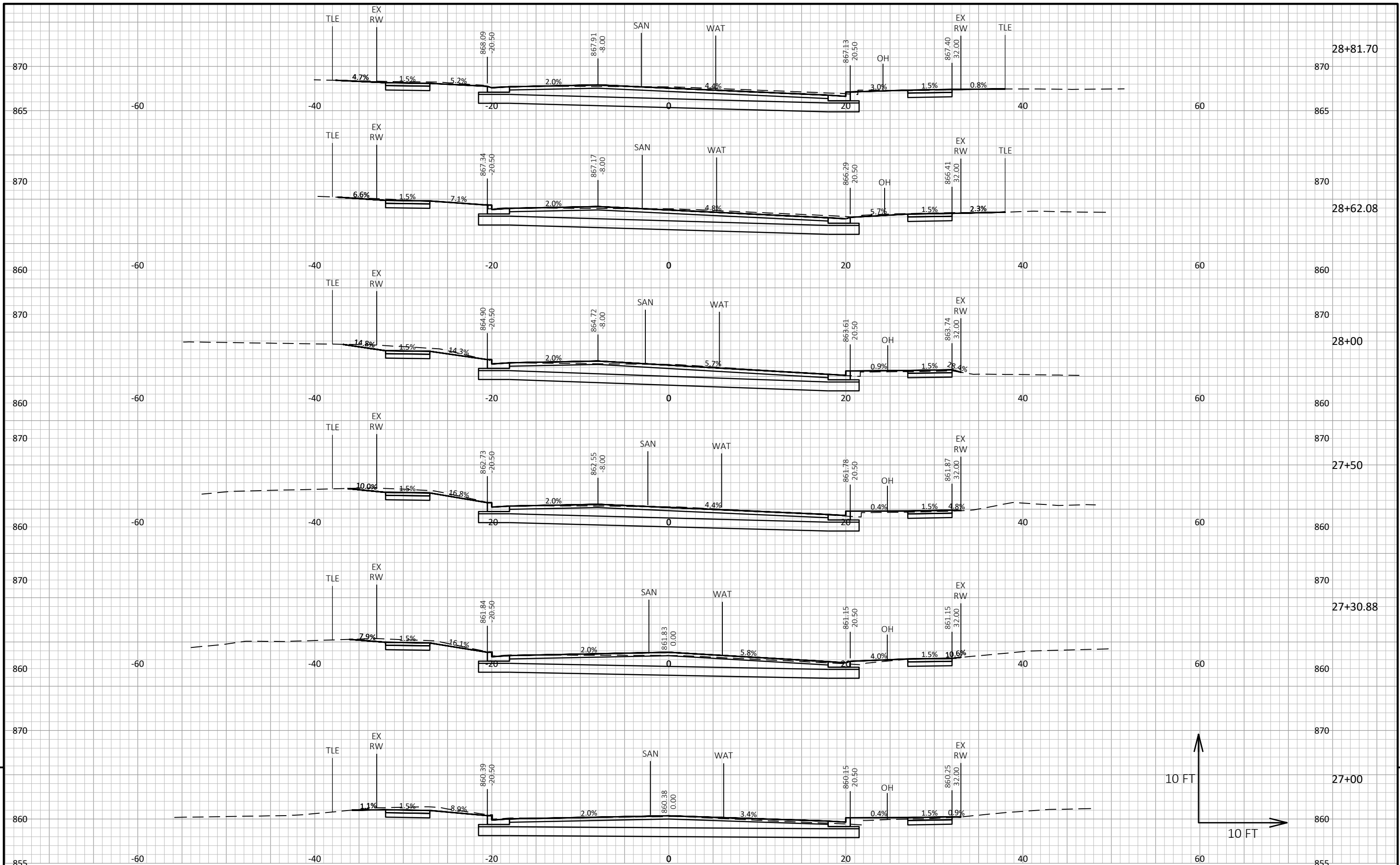
CROSS SECTIONS: SECTION VIEW GROUP - 1

SHEET 105

E



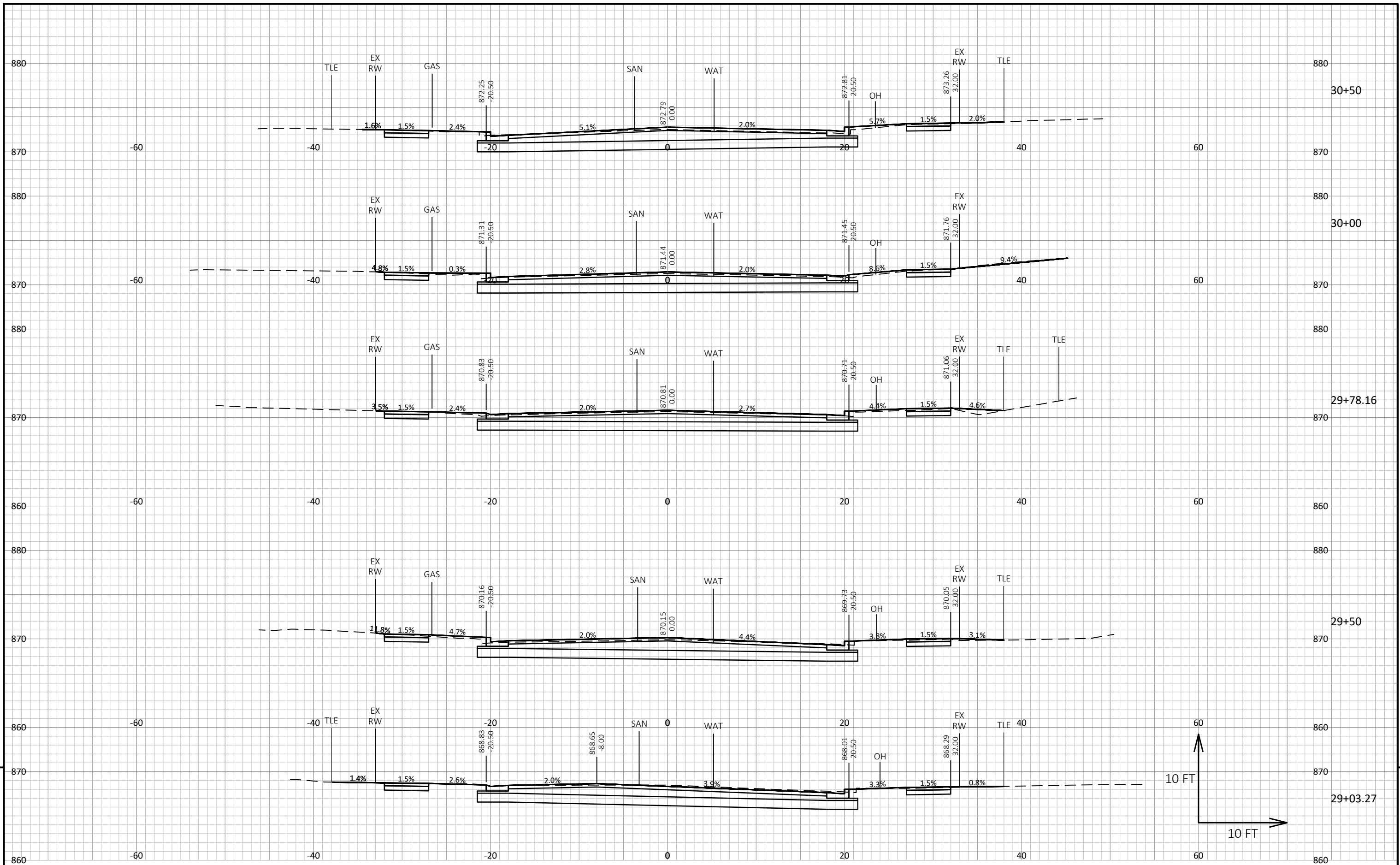
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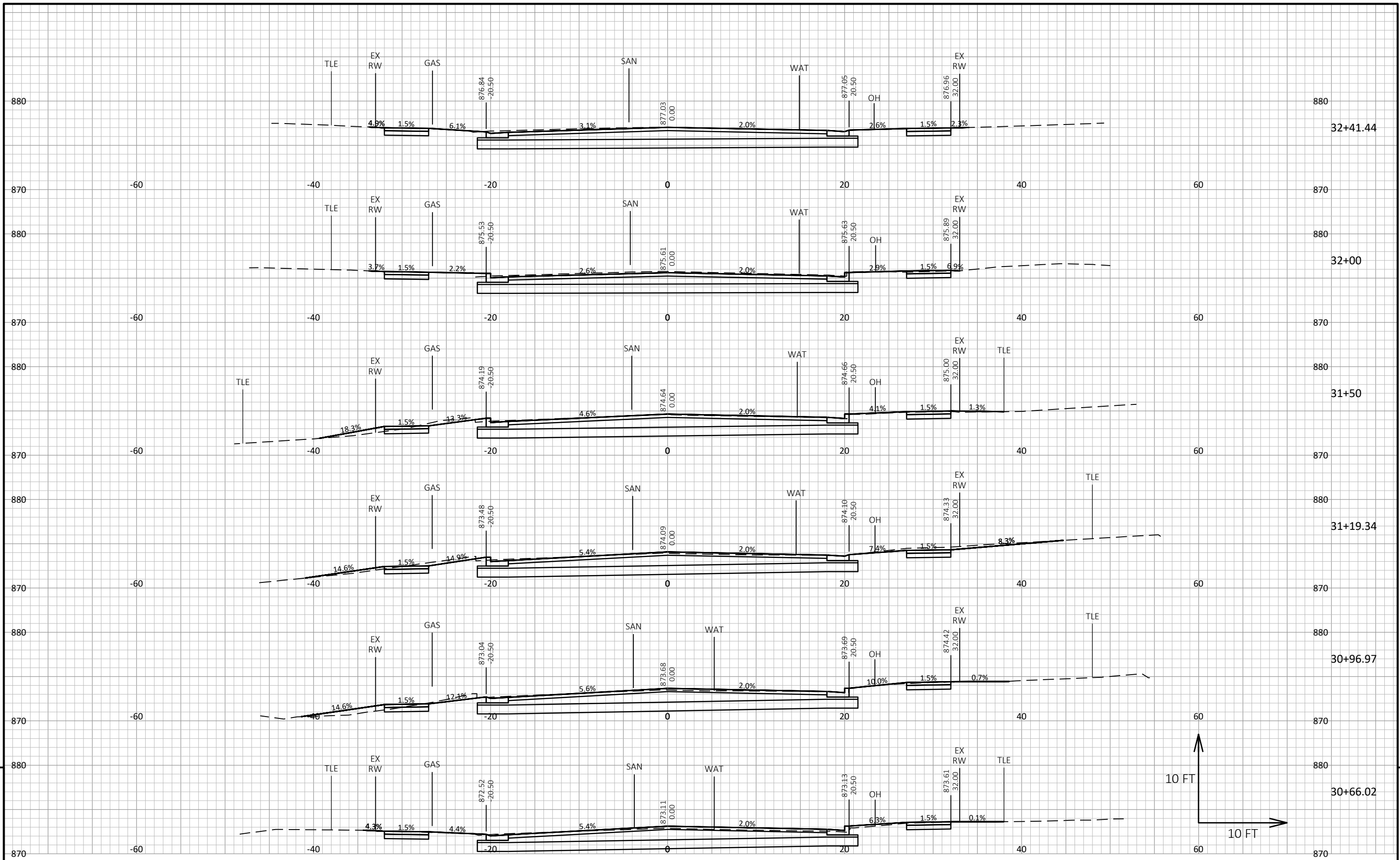
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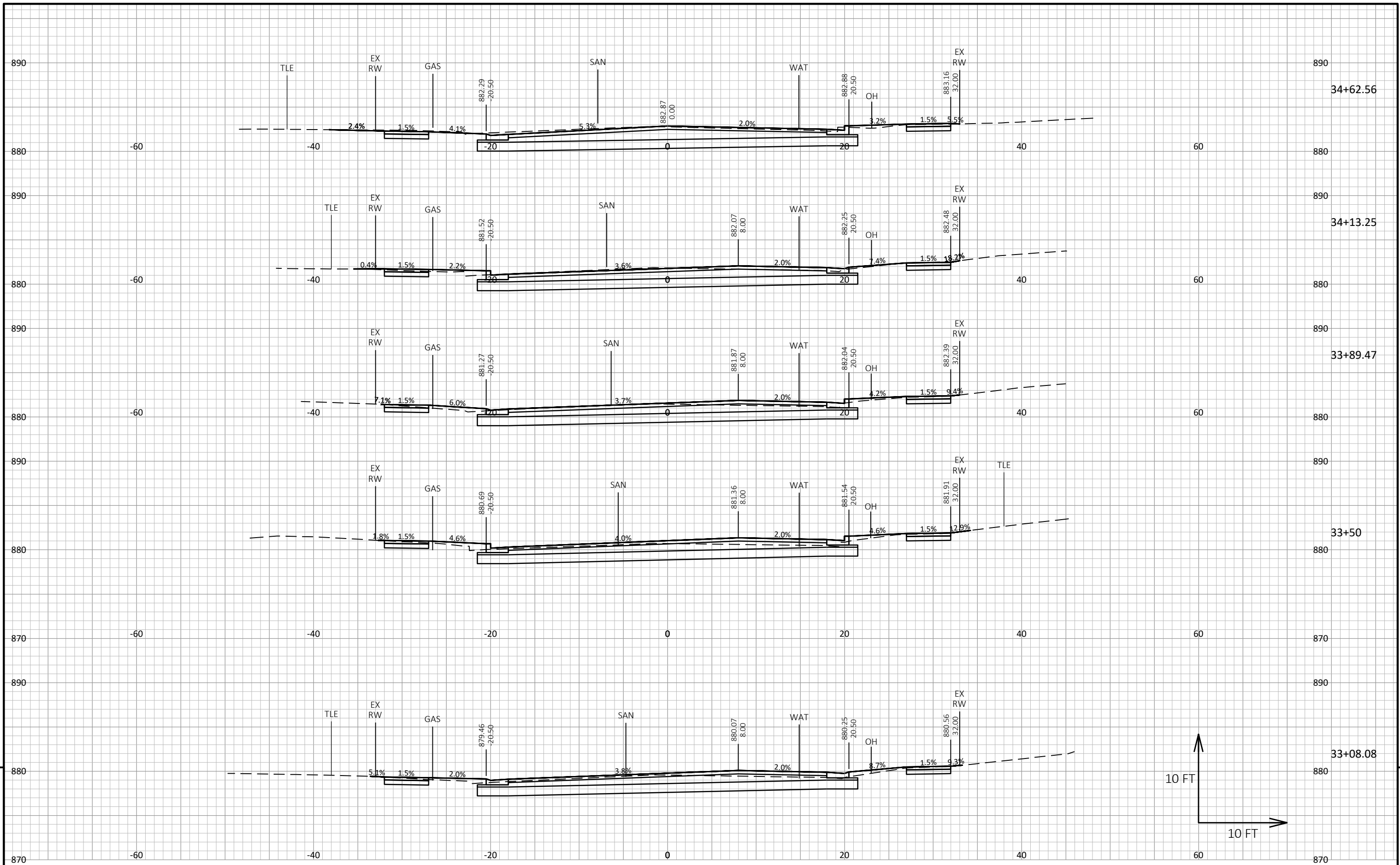
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PROJECT NO: 5685-00-05 HWY: EXCHANGE STREET COUNTY: DANE CROSS SECTIONS: SECTION VIEW GROUP - 1 SHEET 108 E

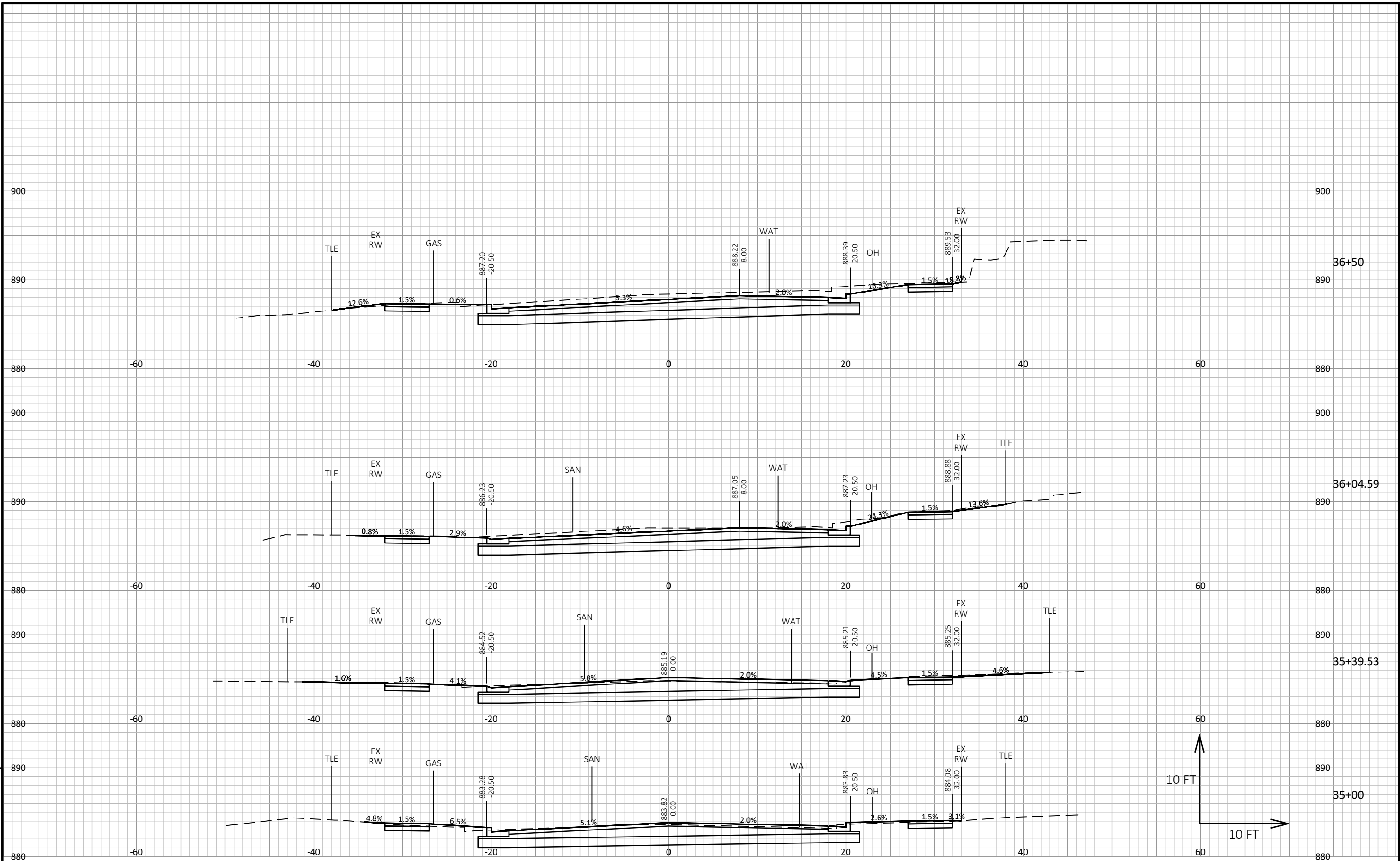


PROJECT NO: 5685-00-05 HWY: EXCHANGE STREET COUNTY: DANE CROSS SECTIONS: SECTION VIEW GROUP - 1 SHEET 109 E



PROJECT NO: 5685-00-05 HWY: EXCHANGE STREET COUNTY: DANE CROSS SECTIONS: SECTION VIEW GROUP - 1 SHEET 110 E

9 9



PROJECT NO: 5685-00-05

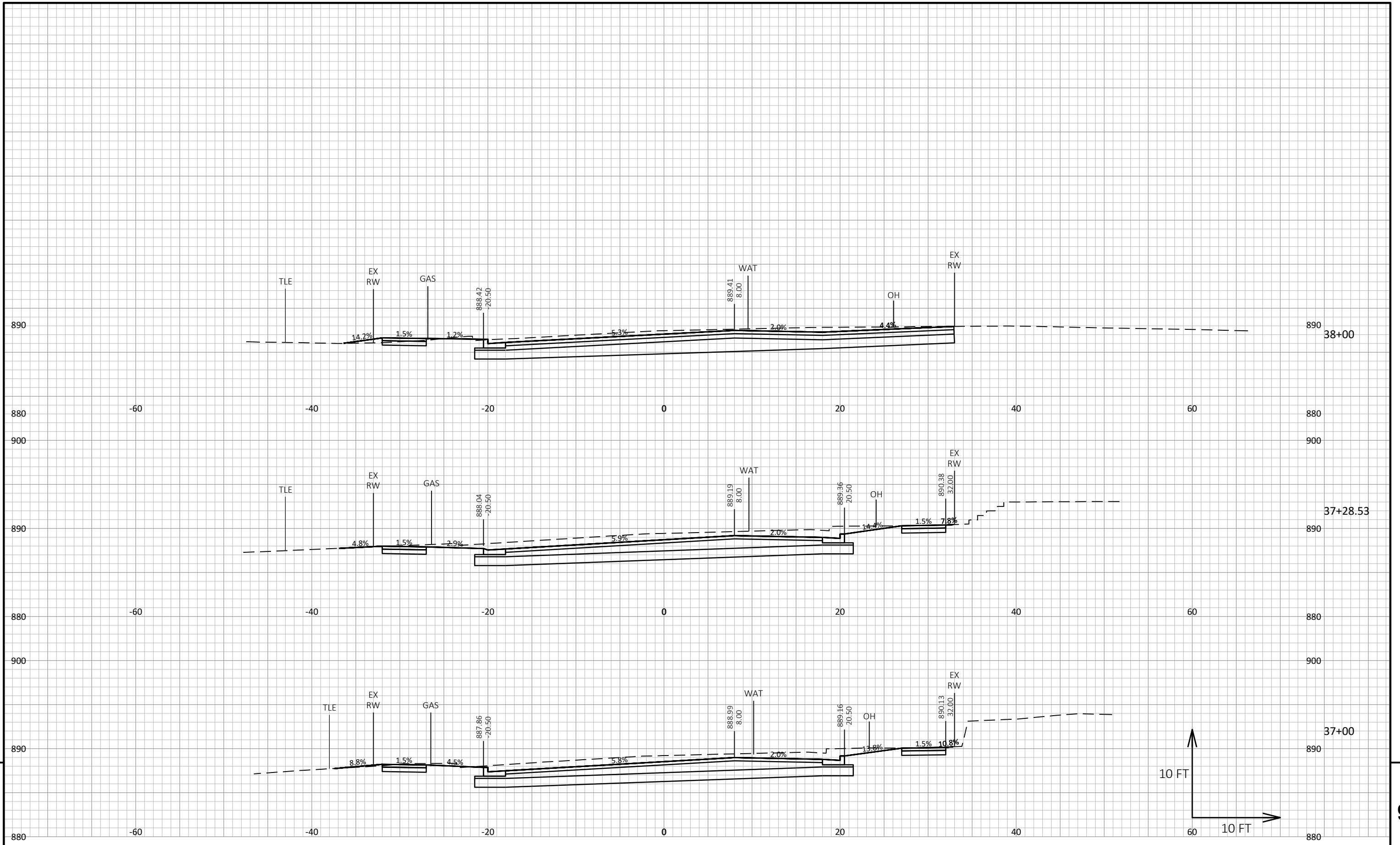
HWY: EXCHANGE STREET

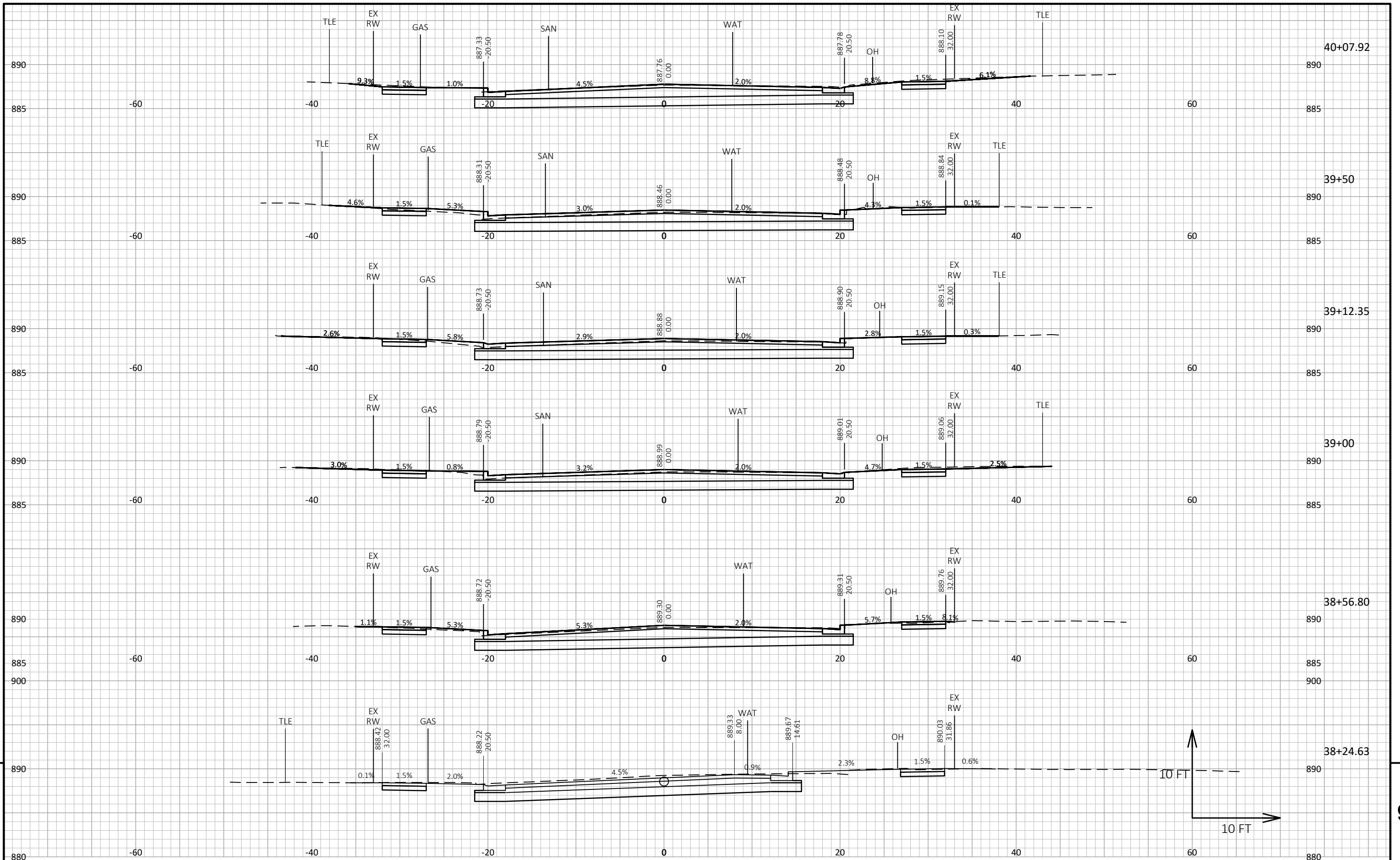
COUNTY: DANE

CROSS SECTIONS: SECTION VIEW GROUP - 1

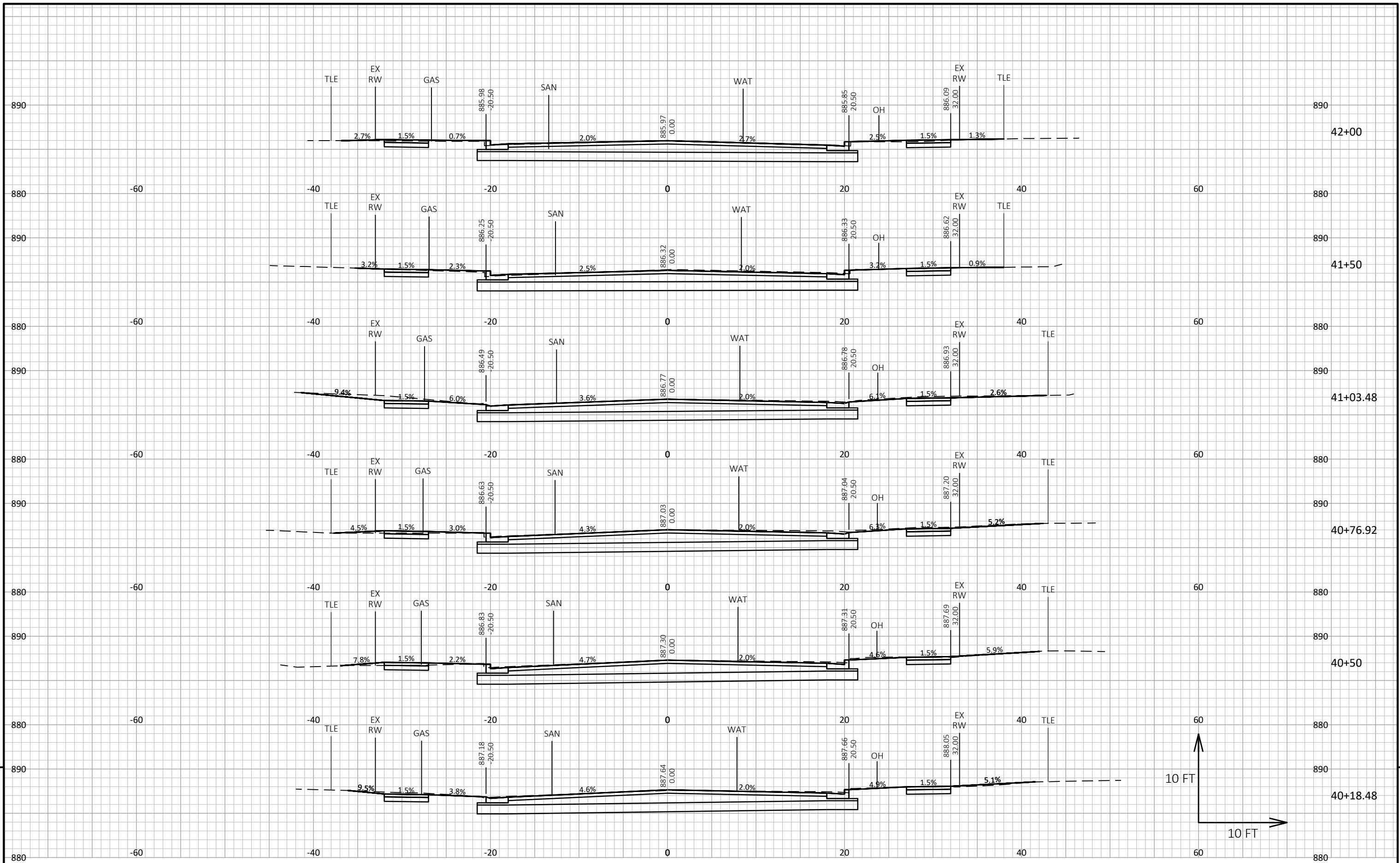
SHEET 111

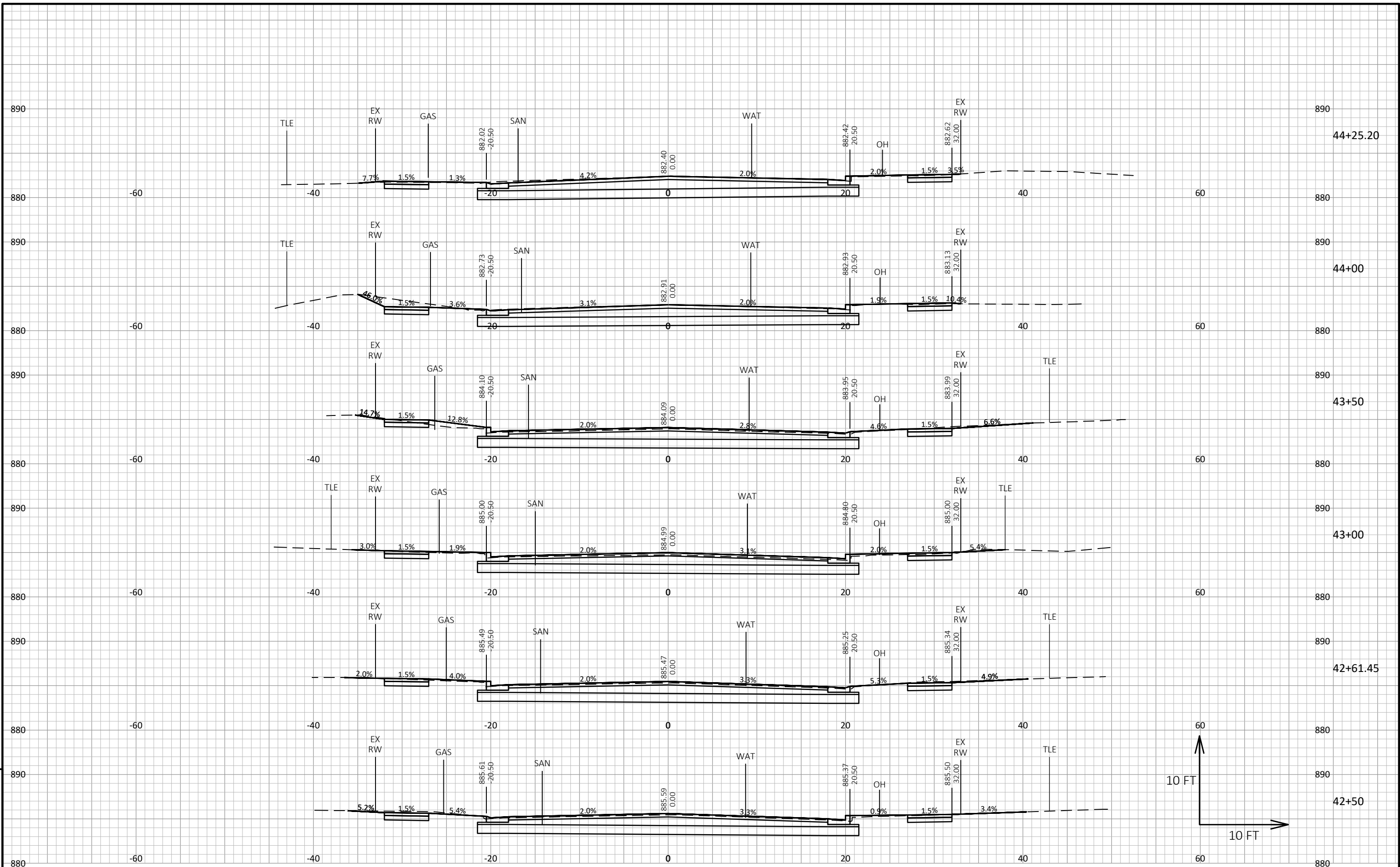
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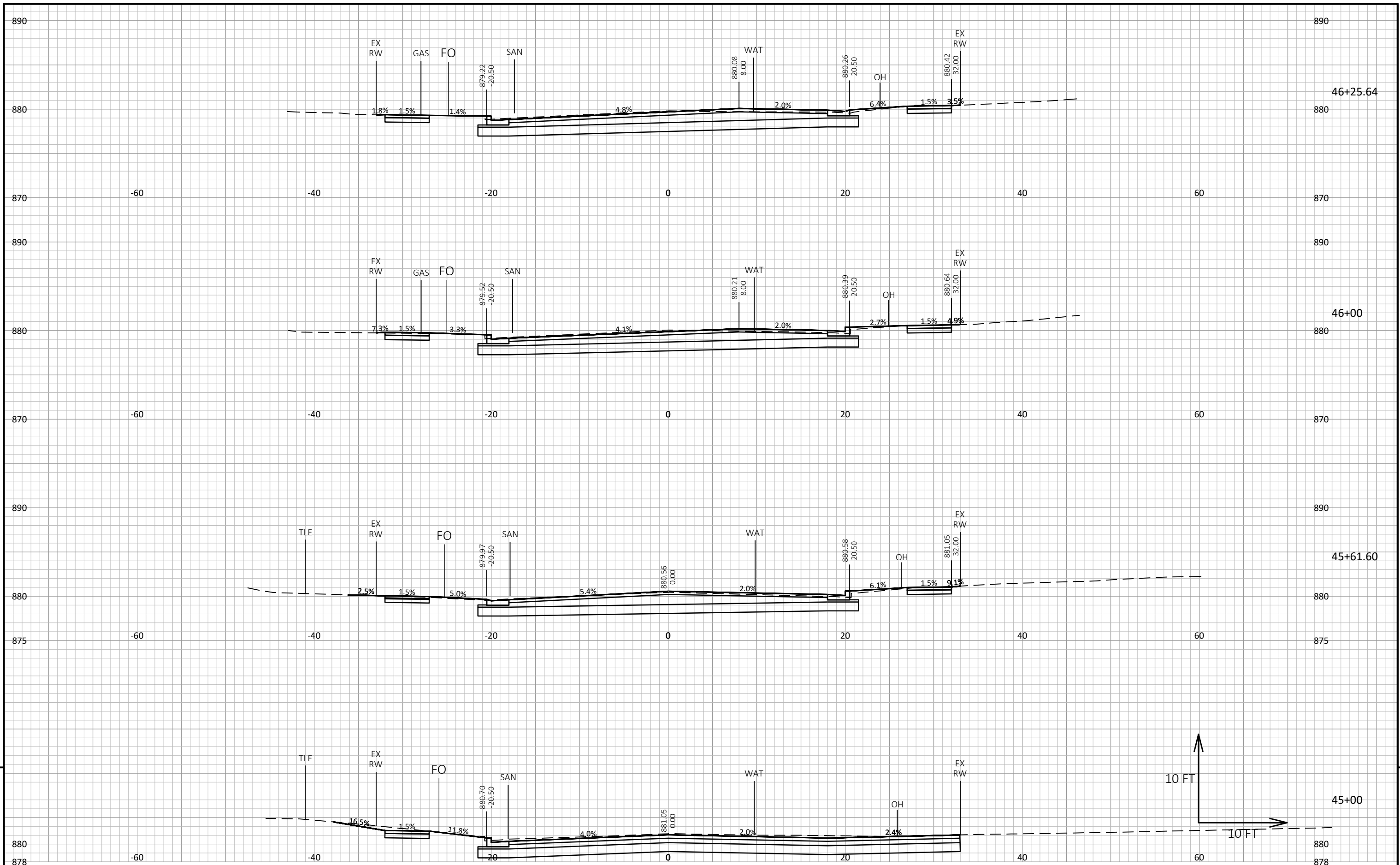


PROJECT NO: 5685-00-05 HWY: EXCHANGE STREET COUNTY: DANE CROSS SECTIONS: SECTION VIEW GROUP - 1 SHEET 113 E

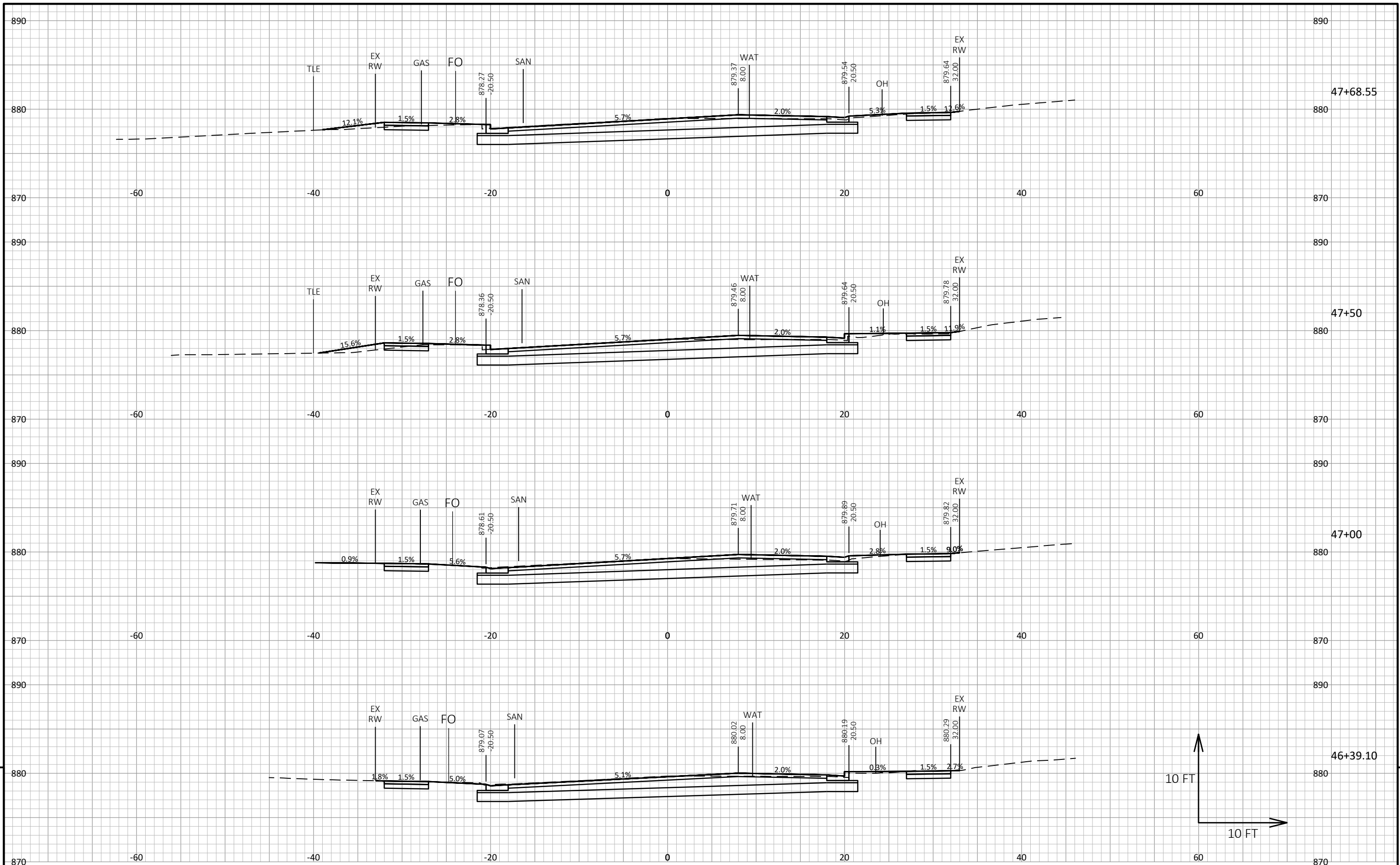




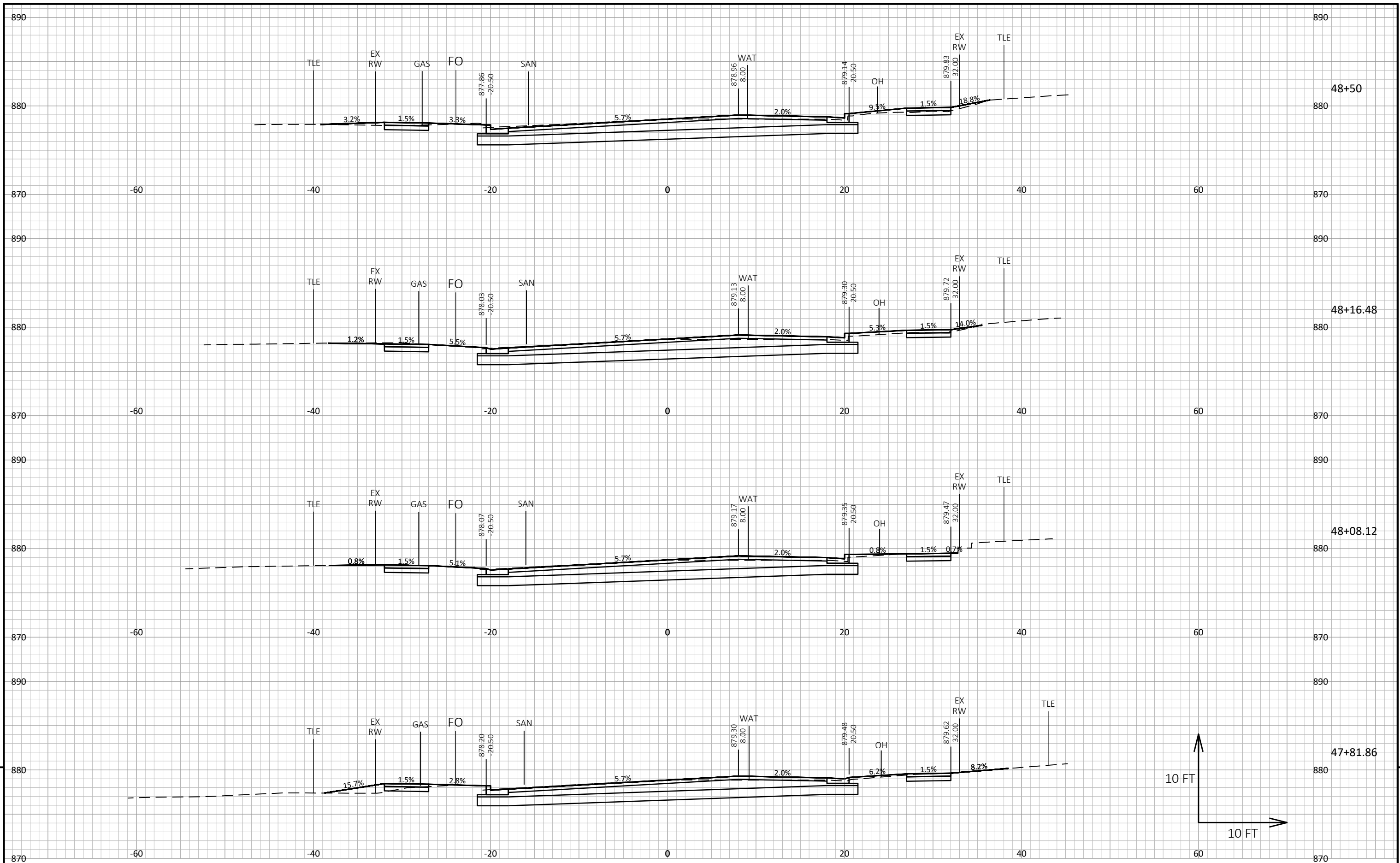
PROJECT NO: 5685-00-05 HWY: EXCHANGE STREET COUNTY: DANE CROSS SECTIONS: SECTION VIEW GROUP - 1 SHEET 115



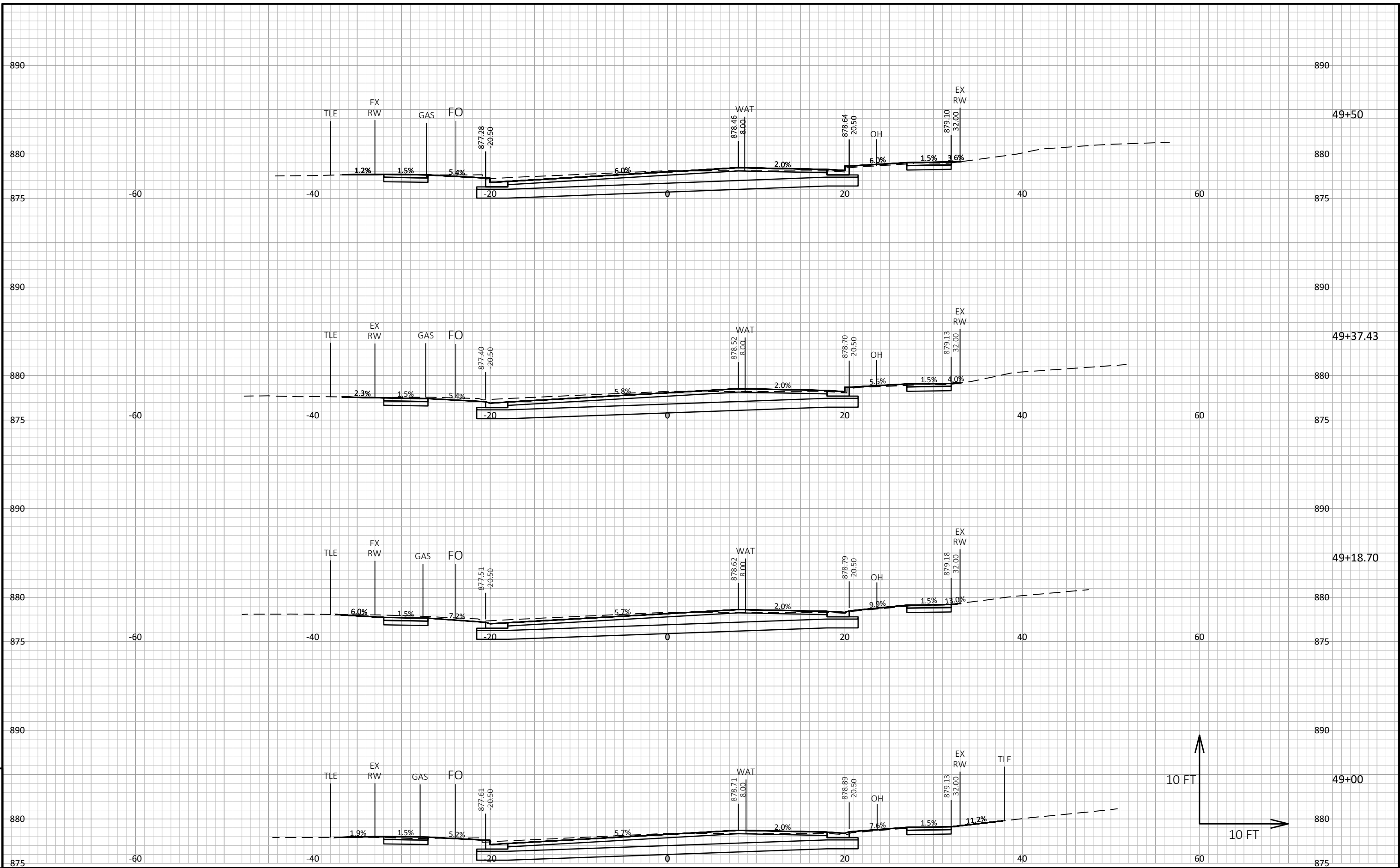
PROJECT NO: 5685-00-05 | HWY: EXCHANGE STREET | COUNTY: DANE | CROSS SECTIONS: SECTION VIEW GROUP - 1 | SHEET 116



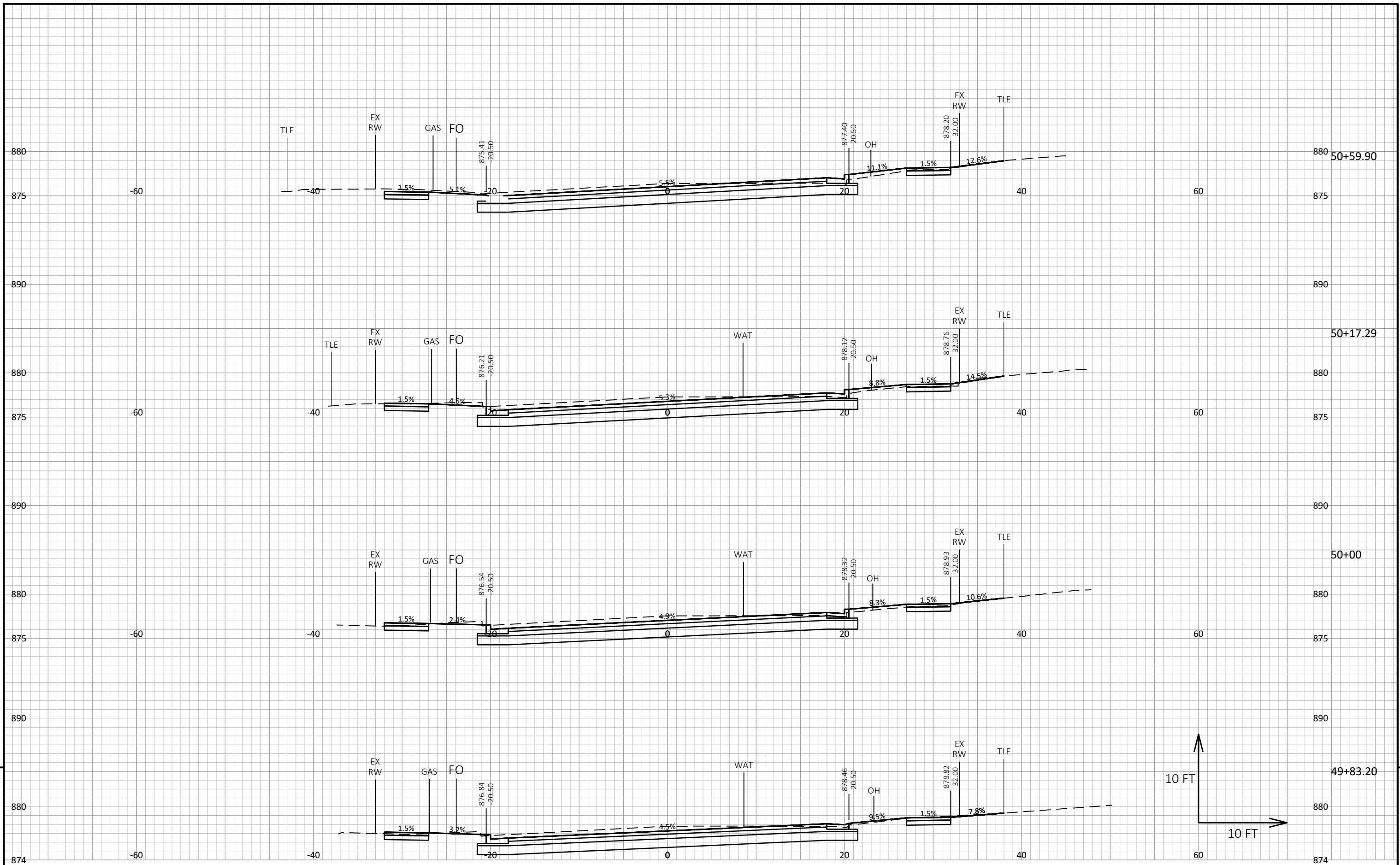
PROJECT NO: 5685-00-05 HWY: EXCHANGE STREET COUNTY: DANE CROSS SECTIONS: SECTION VIEW GROUP - 1 SHEET 117

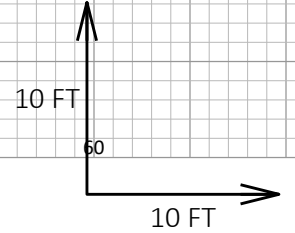
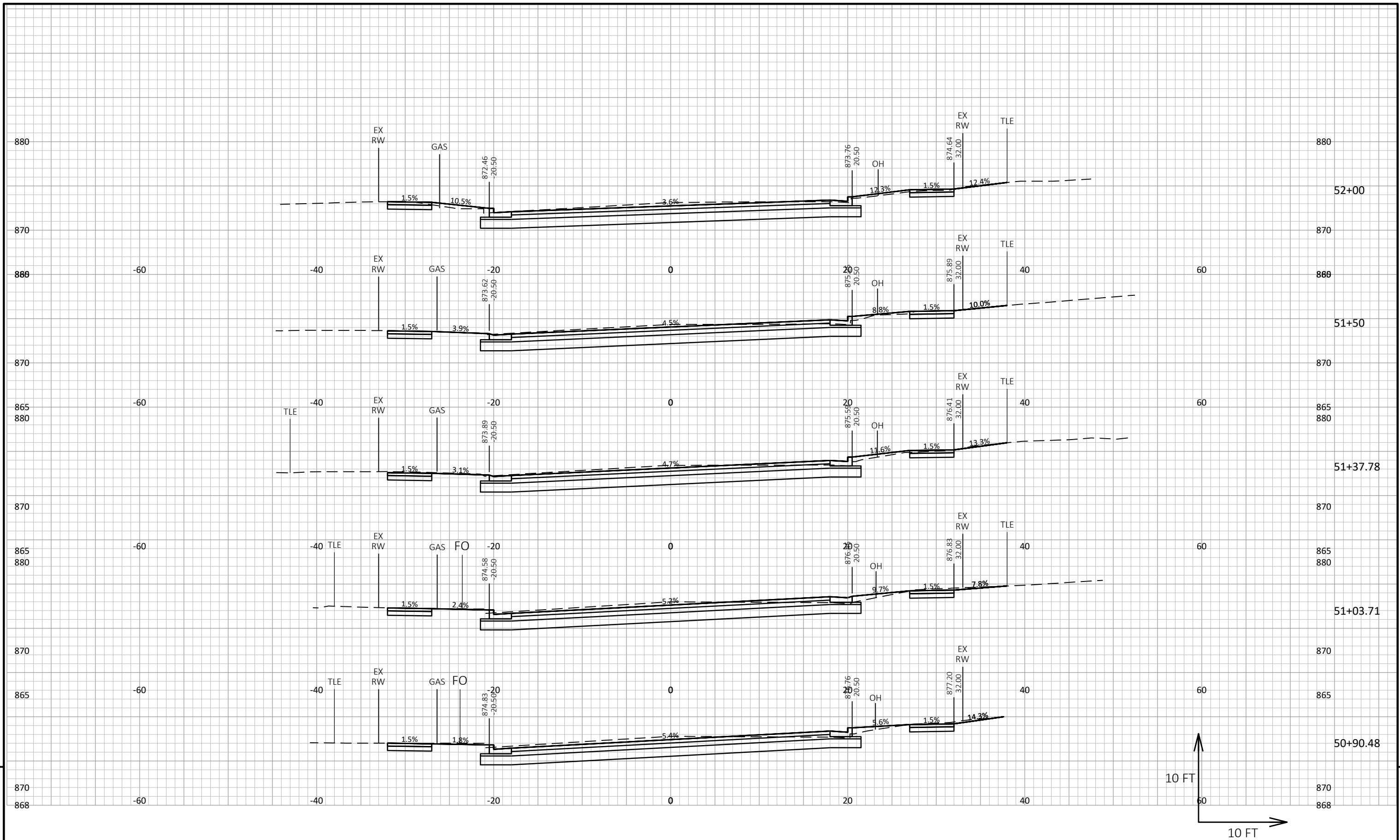


PROJECT NO: 5685-00-05 HWY: EXCHANGE STREET COUNTY: DANE CROSS SECTIONS: SECTION VIEW GROUP - 1 SHEET 118 E



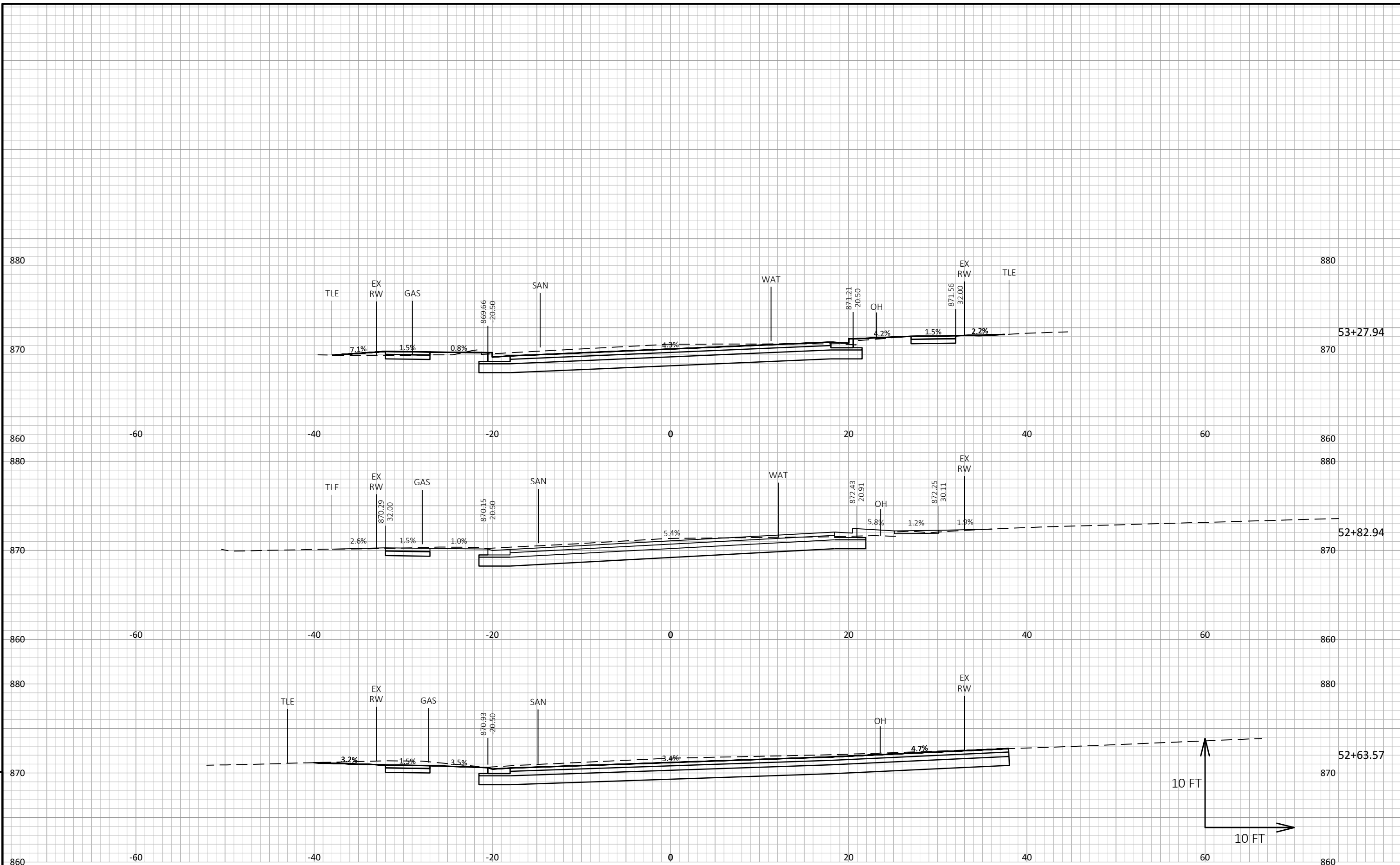
PROJECT NO: 5685-00-05 | HWY: EXCHANGE STREET | COUNTY: DANE | CROSS SECTIONS: SECTION VIEW GROUP - 1 | SHEET 119

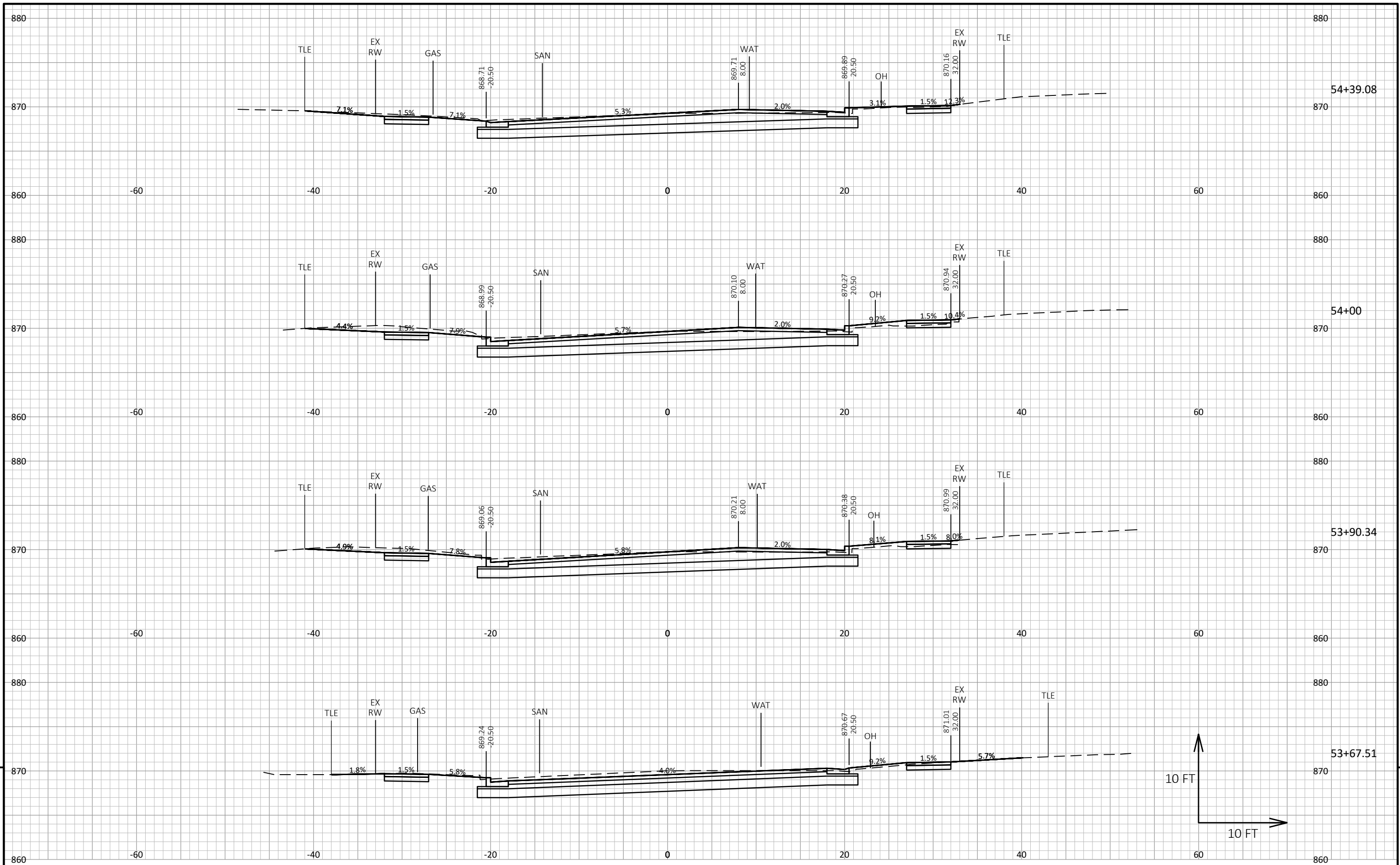




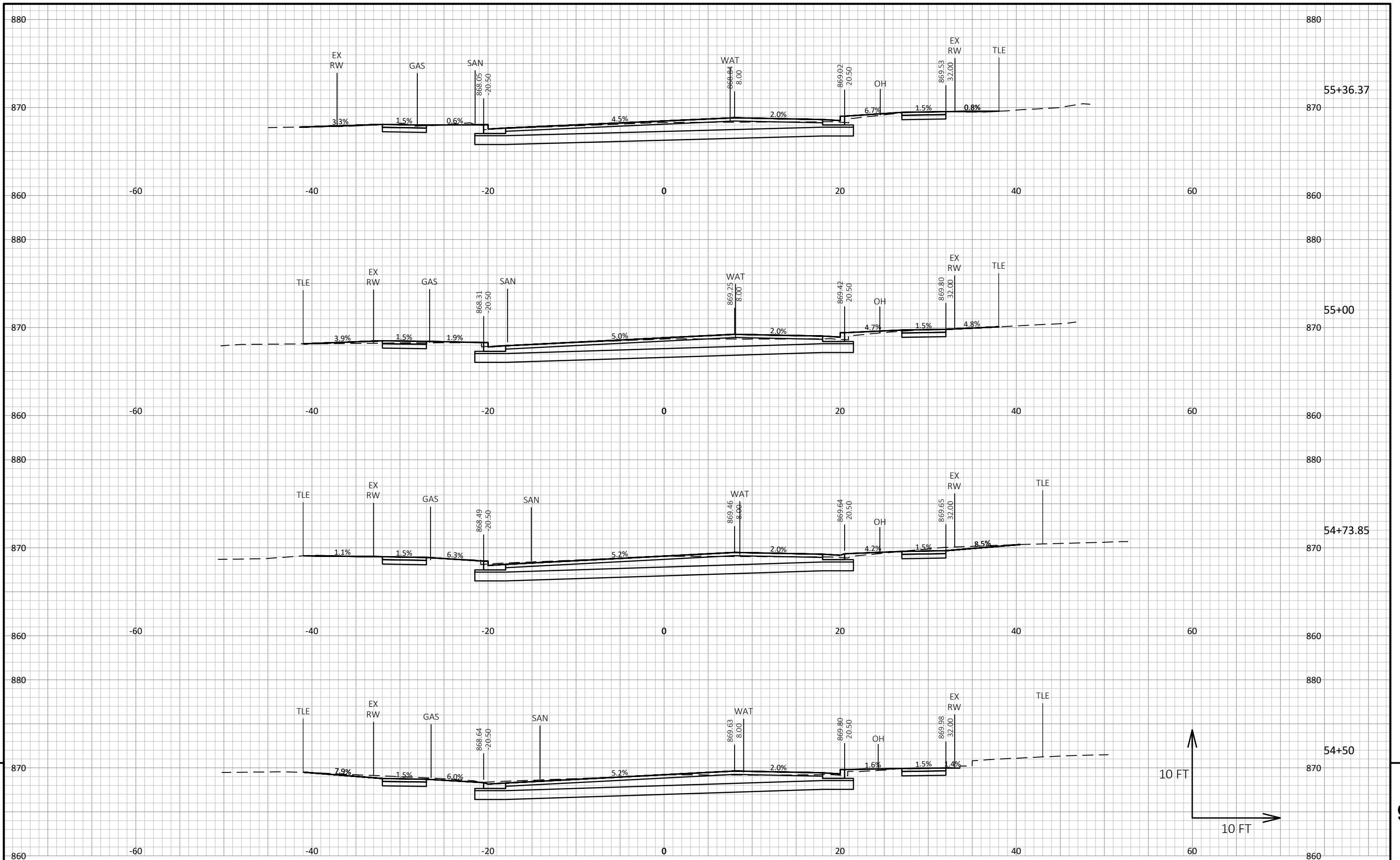
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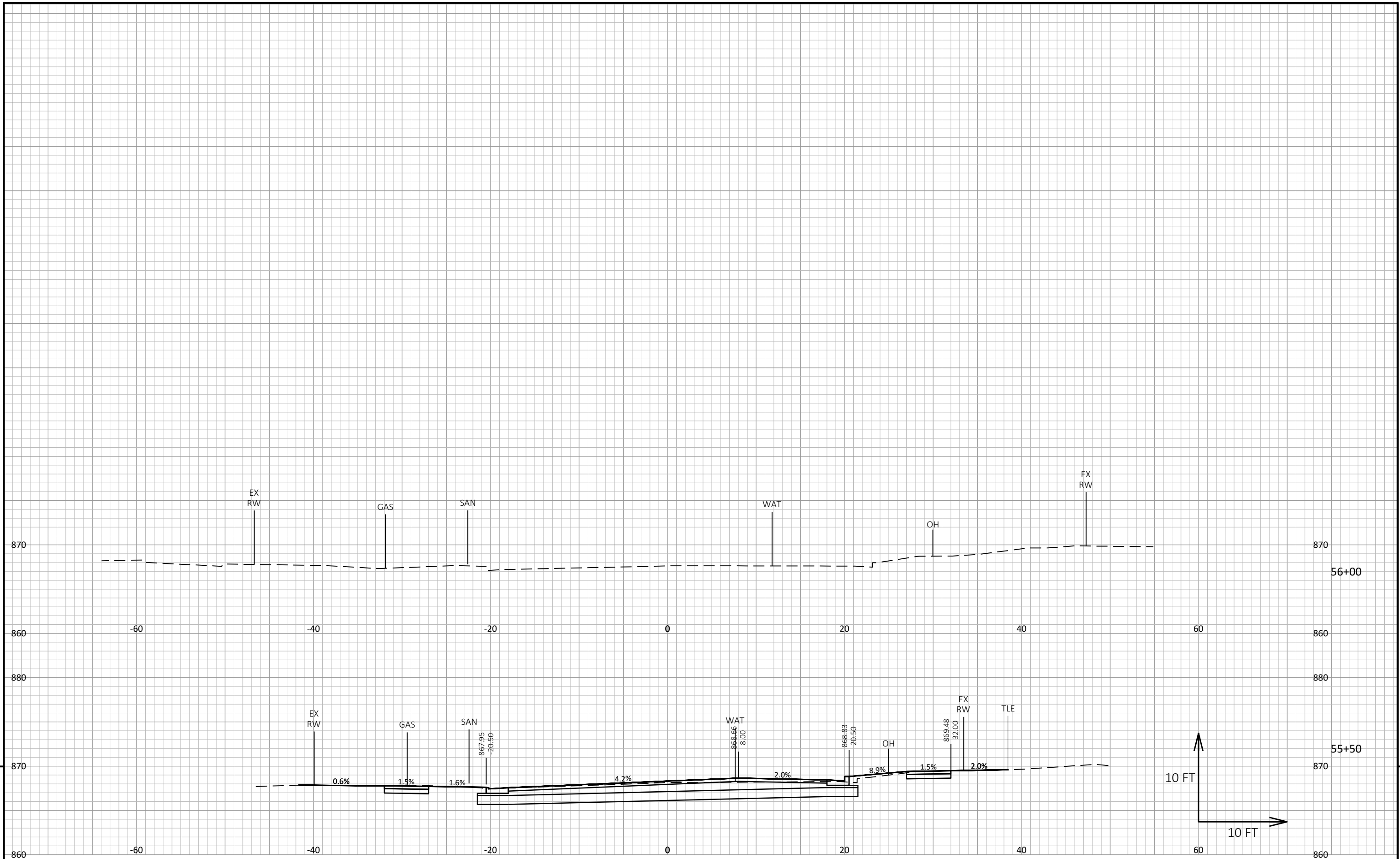




PROJECT NO: 5685-00-05 HWY: EXCHANGE STREET COUNTY: DANE CROSS SECTIONS: SECTION VIEW GROUP - 1 SHEET 123 E



PROJECT NO: 5685-00-05 HWY: EXCHANGE STREET COUNTY: DANE CROSS SECTIONS: SECTION VIEW GROUP - 1 SHEET 124 E



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PROJECT NO: 5685-00-05	HWY: EXCHANGE STREET	COUNTY: DANE	CROSS SECTIONS: SECTION VIEW GROUP - 1	SHEET 125	E
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Wisconsin Department of Transportation

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