

RHI

FEBRUARY 2024

ORDER OF SHEETS

Section No.	1	Title
Section No.	2	Typical Sections and Details
Section No.	3	Estimate of Quantities
Section No.	3	Miscellaneous Quantities
Section No.	4	Right of Way Plat
Section No.	5	Plan and Profile
Section No.	6	Standard Detail Drawings
Section No.	7	Sign Plates
Section No.	8	Structure Plans
Section No.	9	Computer Earthwork Data
Section No.	9	Cross Sections

TOTAL SHEETS = 98



26

DESIGN DESIGNATION

A.A.D.T. (2026)	=	4100
A.A.D.T. (2046)	=	4500
D.H.V.	=	13.6
D.D.	=	54/46
T.	=	17.5
DESIGN SPEED	=	60 MPH
ESALS	=	2,600,000

CONVENTIONAL SYMBOLS

PLAN

CORPORATE LIMITS	
PROPERTY LINE	
LOT LINE	
LIMITED HIGHWAY EASEMENT	
EXISTING RIGHT OF WAY	
PROPOSED OR NEW R/W LINE	
SLOPE INTERCEPT	
REFERENCE LINE	
EXISTING CULVERT	
PROPOSED CULVERT (Box or Pipe)	
COMBUSTIBLE FLUIDS	
MARSH AREA	
WOODED OR SHRUB AREA	

PROFILE

GRADE LINE	
ORIGINAL GROUND	
MARSH OR ROCK PROFILE (To be noted as such)	
SPECIAL DITCH	
GRADE ELEVATION	
CULVERT (Profile View)	
UTILITIES	
ELECTRIC	
FIBER OPTIC	
GAS	
SANITARY SEWER	
STORM SEWER	
TELEPHONE	
WATER	
UTILITY PEDESTAL	
POWER POLE	
TELEPHONE POLE	

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

RHINELANDER - MONICO

STH 17 NORTH TO CTH P

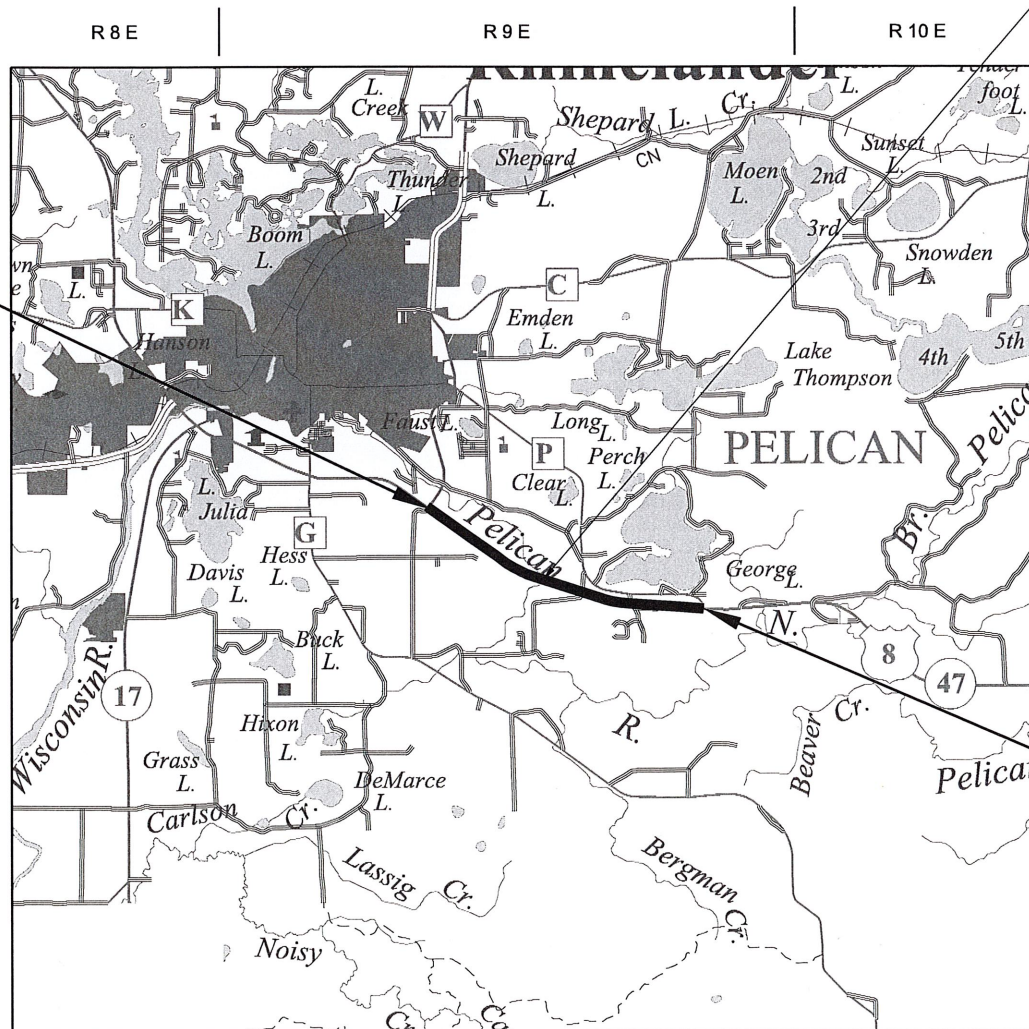
USH 8

ONEIDA COUNTY

STATE PROJECT NUMBER

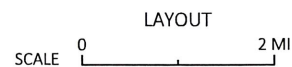
1590-12-76

NET EXCEPTION TO CL LENGTH
STA 306+36.38EB - STA 307+35.33
STRUCTURE B-43-24



BEGIN PROJECT
STA 242+00
Y = 155751.577
X = 272017.433

END PROJECT
STA 402+80



TOTAL NET LENGTH OF CENTERLINE = 3.027 MI

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS), ONEIDA COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES. ELEVATIONS ARE REFERENCED TO NAVD 88 (2012). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 18.

STATE PROJECT

1590-12-76

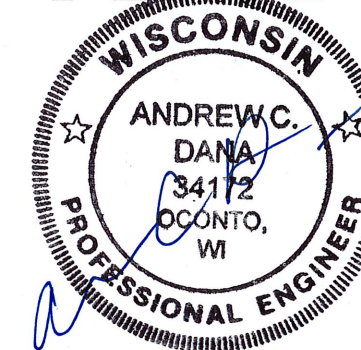
FEDERAL PROJECT

PROJECT

CONTRACT

ORIGINAL PLANS PREPARED BY

AYRES



10-16-2023
(Date)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY

Surveyor	WISDOT
Designer	AYRES
Project Manager	STACY HAGENBUCHER
Regional Examiner	FRED SCHUNKE
Regional Supervisor	KAI KILEN

APPROVED FOR THE DEPARTMENT

DATE: 10/25/2023

E

GENERAL NOTES

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLAN ARE APPROXIMATE. THERE MAY BE OTHER INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

THE CONTRACTOR SHALL NOTIFY DIGGERS HOTLINE AND AFFECTED UTILITIES PRIOR TO THE START OF WORK. ANY LOCAL MUNICIPAL UTILITY WHICH IS NOT A MEMBER OF THE DIGGERS HOTLINE MUST BE CONTACTED SEPARATELY.

CURVE DATA IS BASED ON ARC DEFINITION.

BEARINGS SHOWN ON THIS PLAN ARE TRUE BEARINGS.

PROPERTY LINES SHOWN ARE APPROXIMATE.

ANY DOWEL BARS, TIE BARS, REINFORCEMENT, OR TERMINAL ANCHORS, FOUND IN EXISTING PAVEMENT WILL BE INCIDENTAL TO THE ITEM "REMOVING PAVEMENT".

UTILITY CONTACTS

* SPECTRUM - COMMUNICATION LINE
ATTN: HAROLD C MOORE
821 LINCOLN STREET
RHINELANDER, WI 54501
PHONE: 715-216-0348
EMAIL: harold.moore@charter.com

* WISCONSIN PUBLIC SERVICE CORPORATION - ELECTRICITY
ATTN: KEVIN TERMAAT
P.O. BOX 1106
WAUSAU, WI 54401-1166
PHONE: 715-848-7353
EMAIL: kevin.termaat@wisconsinpublicservice.com

* WISCONSIN PUBLIC SERVICE CORPORATION - GAS
ATTN: SHANE SARKKINEN
2027 NAVAJO STREET
RHINELANDER, WI 54501
PHONE: 715-369-7133
EMAIL: shane.sarkkinen@wisconsinpublicservice.com

*-MEMBER OF DIGGERS HOTLINE

WISCOSIN DEPARTMENT OF NATURAL RESOURCES

NORTH REGION HEADQUARTERS
ATTN: WENDY HENNIGES
107 SUTLIFF
RHINELANDER, WI 54501
PHONE: 715-365-8916
EMAIL: wendy.henniges@wisconsin.gov



RUNOFF COEFFICIENT TABLE

LAND USE:	HYDROLOGIC SOIL GROUP											
	A			B			C			D		
	SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)		
	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER
ROW CROPS	.08	.16	.22	.12	.20	.27	.15	.24	.33	.19	.28	.38
	.22	.30	.38	.26	.34	.44	.30	.37	.50	.34	.41	.56
MEDIAN STRIP-TURF	.19	.20	.24	.19	.22	.26	.20	.23	.30	.20	.25	.30
	.24	.26	.30	.25	.28	.33	.26	.30	.37	.27	.32	.40
SIDE SLOPE-TURF			.25			.27			.28			.30
			.32			.34			.36			.38
PAVEMENT:												
ASPHALT	.70 - .95											
CONCRETE	.80 - .95											
BRICK	.70 - .80											
DRIVES, WALKS	.75 - .85											
ROOFS	.75 - .95											
GRAVEL ROADS, SHOULDERS	.40 - .60											

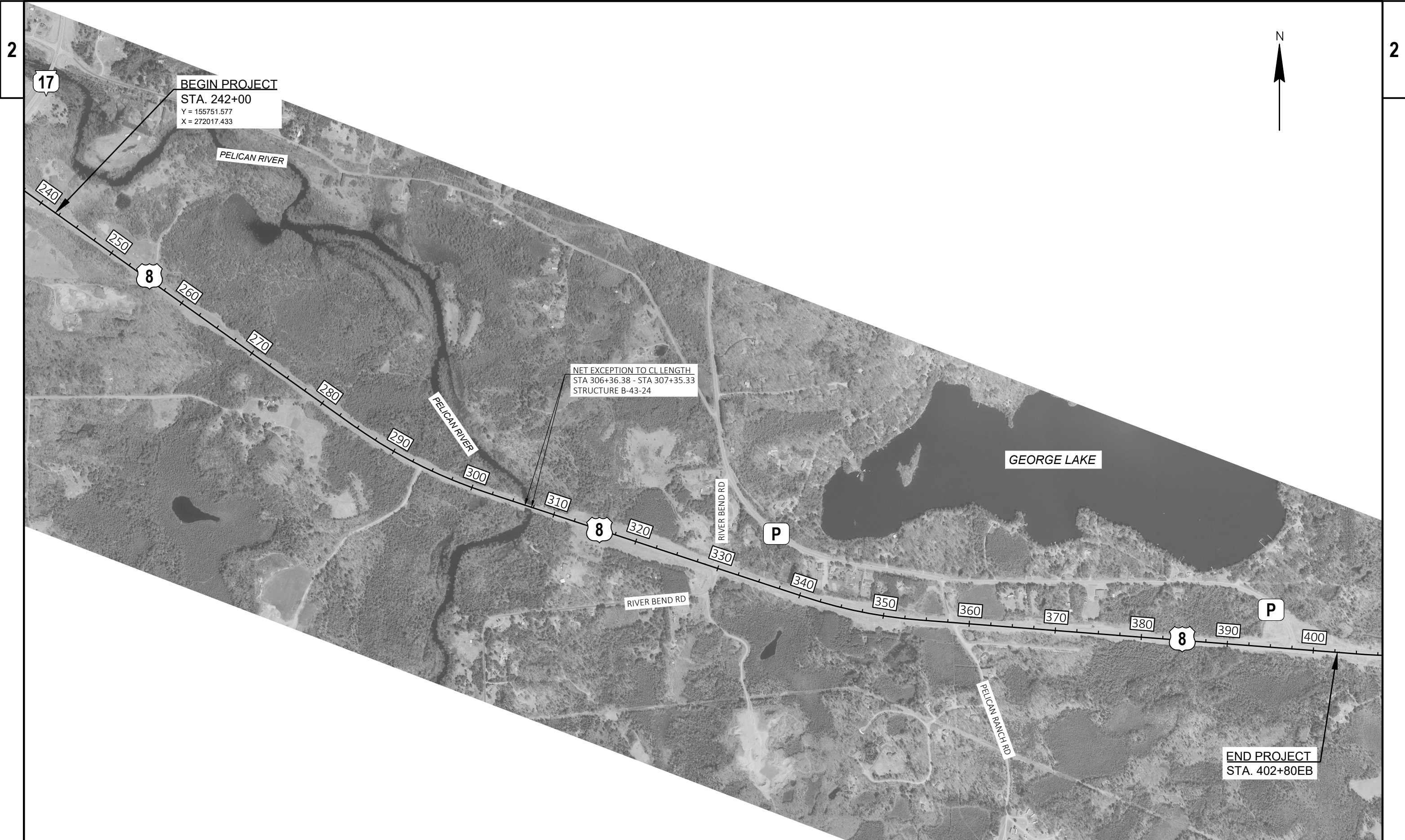
TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = 11.19 ACRES
SOIL GROUP A, B, A/D, B/D.

STANDARD ABBREVIATIONS

ADT	AVERAGE DAILY TRAFFIC	NC	NORMAL CROWN
AC	ASPHALT CEMENT	PT	POINT OF TANGENCY
AGG	AGGREGATE	PC	POINT OF CURVATURE
ASPH	ASPHALT	PI	POINT OF INTERSECTION
BM	BENCH MARK	PE	PRIVATE ENTRANCE
C/L	CENTERLINE	R	RADIUS
CONC	CONCRETE	REM	REMOVE
CMP	CORRUGATED METAL PIPE	R/L OR RL	REFERENCE LINE
CR.	CREEK	RCCP	REINFORCED CONCRETE CULVERT PIPE
D	DEGREE OF CURVE	RCPSS	REINFORCED CONCRETE PIPE STORM SEWER
DHV	DESIGN HOUR VOLUME	R.O.	RUNOUT
ESALS	EQUIVALENT SINGLE AXIS LOADS	R/W	RIGHT-OF-WAY
EXIST	EXISTING	STA	STATION
FE	FIELD ENTRANCE	SE	SUPER ELEVATION
HYD	HYDRANT	SS	STORM SEWER
IP	IRON PIPE OR PIN	T	TANGENT
L	LENGTH OF CURVE	TEL	TELEPHONE
LC	LONG CHORD OF CURVE	TLE	TEMPORARY LIMITED EASEMENT
LR	LENGTH OF RUNOFF	T	TRUCKS
MH	MANHOLE	VC	VERTICAL CURVE
		W	WELL

ORDER OF SECTION 2 SHEETS

- GENERAL NOTES & CONTACTS
- PROJECT OVERVIEW
- TYPICAL SECTIONS
- CONSTRUCTION DETAILS PLAN
- DETAILS
- PLAN GRADES AND OFFSETS
- EROSION CONTROL
- ALIGNMENT DIAGRAMS



PROJECT NO: 1590-12-76

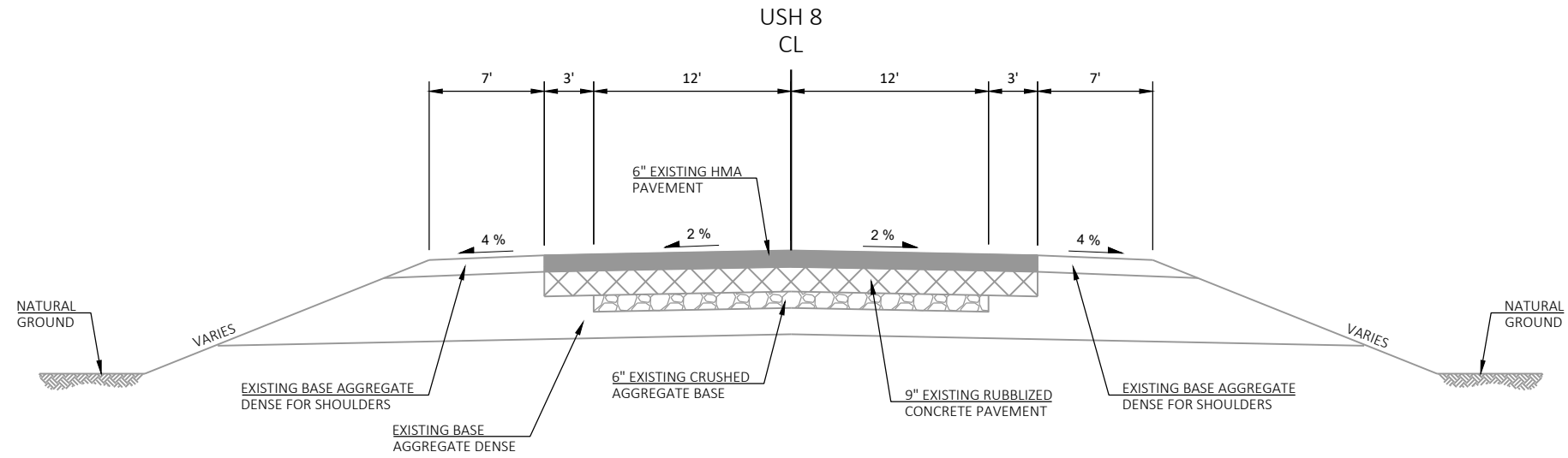
HWY: USH 8

COUNTY: ONEIDA

PROJECT OVERVIEW

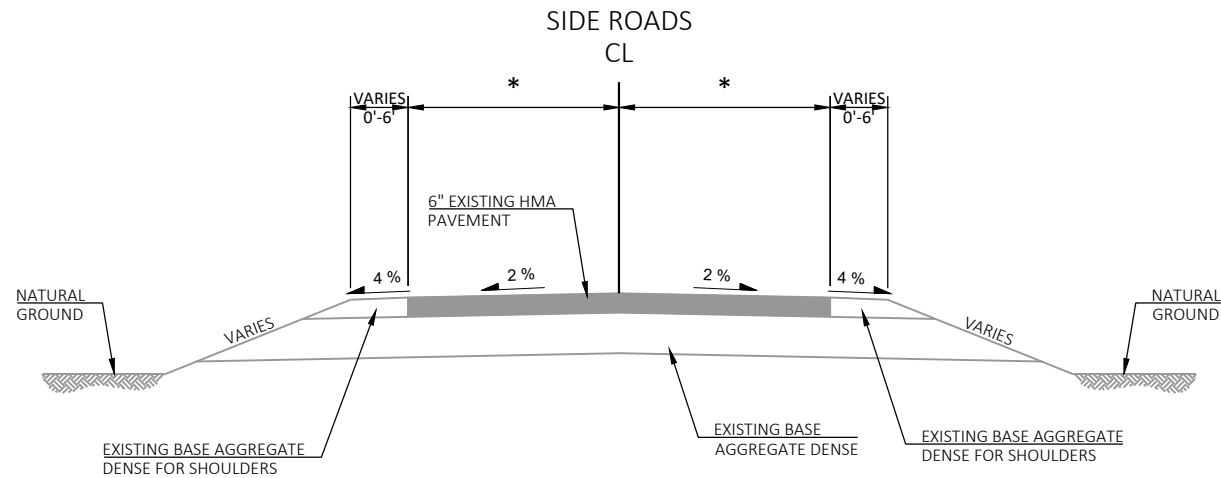
SHEET

E



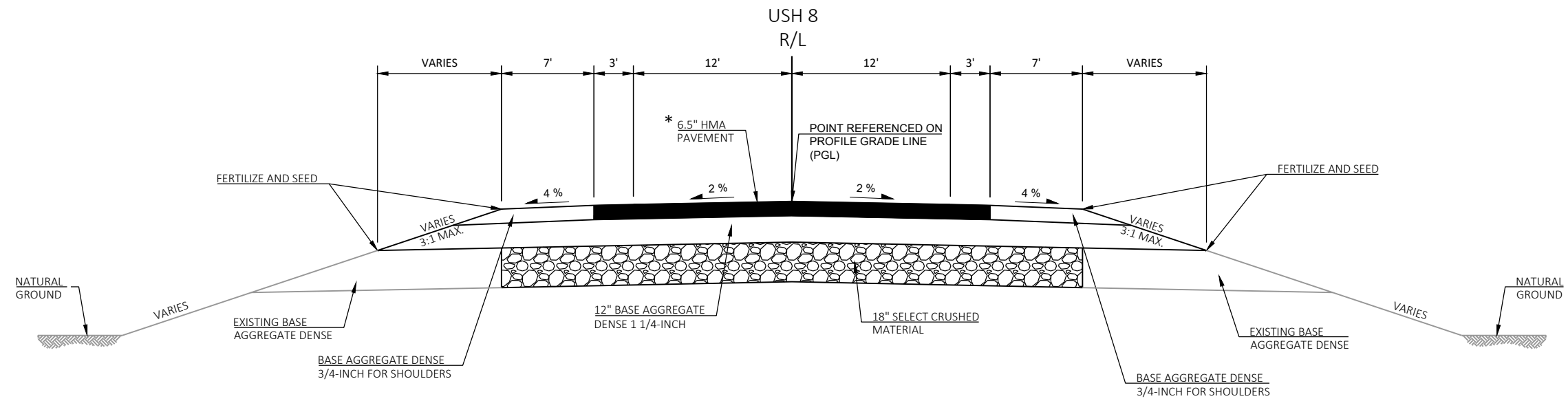
EXISTING TYPICAL SECTION FOR USH 8

STA 242+00 - STA 306+23
STA 307+38 - STA 402+80



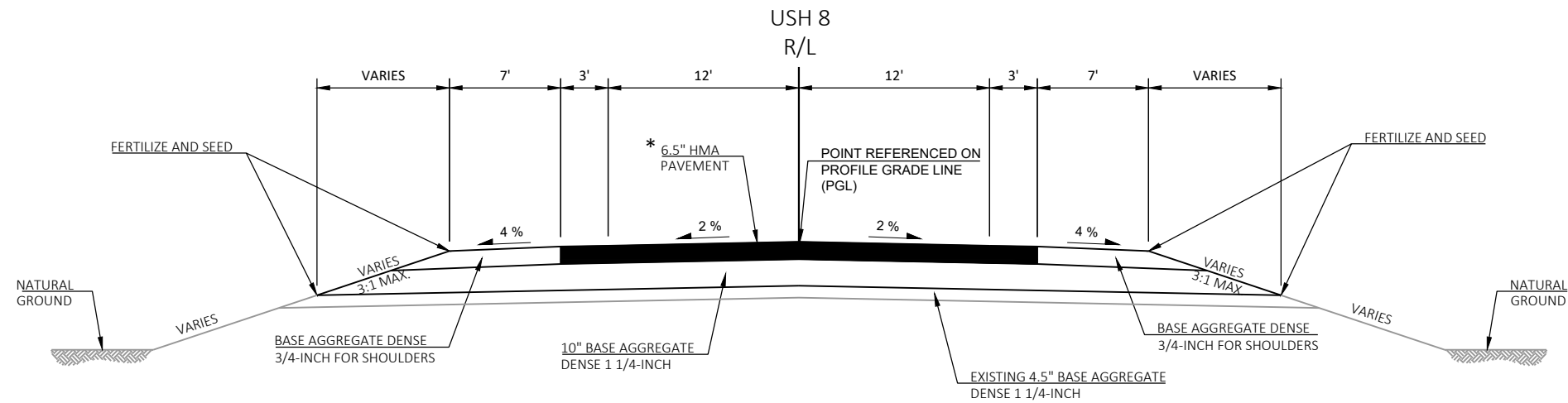
EXISTING TYPICAL SECTION FOR SIDE ROADS

* RIVER BEND ROAD - 11'
PELICAN RANCH ROAD - 11'
CTH P - 12'



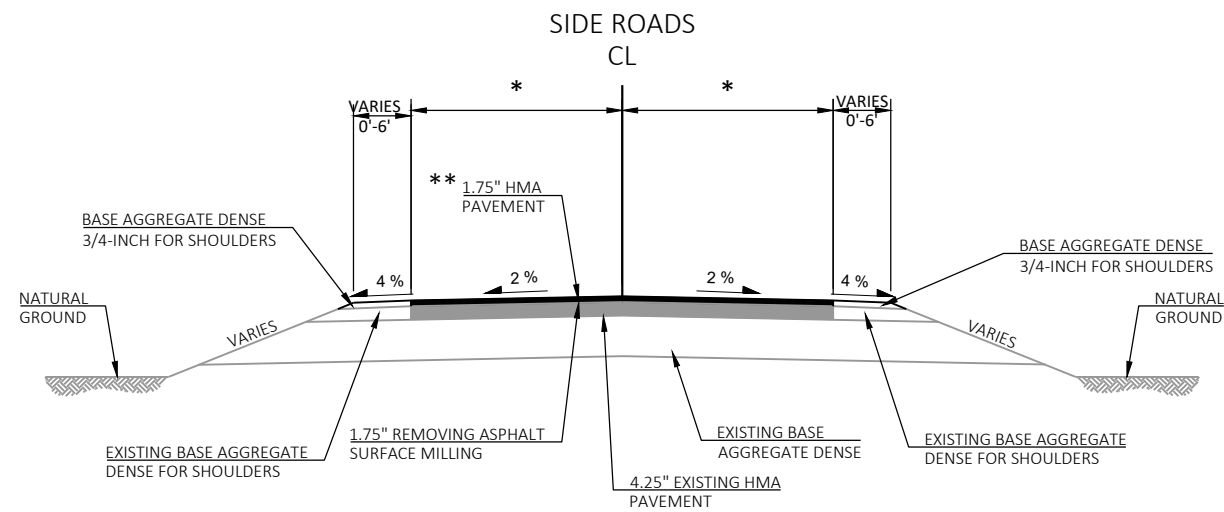
FINISHED TYPICAL SECTION FOR USH 8
 STA 242+00 - STA 306+23
 STA 307+38 - STA 329+00

PAVEMENT LAYERS	
* 6.5" HMA PAVEMENT	1.75" (4 MT 58-34 S) UPPER LAYER 2.25" (3 MT 58-28 S) LOWER LAYER 2.5" (3 MT 58-28 S) LOWER LAYER



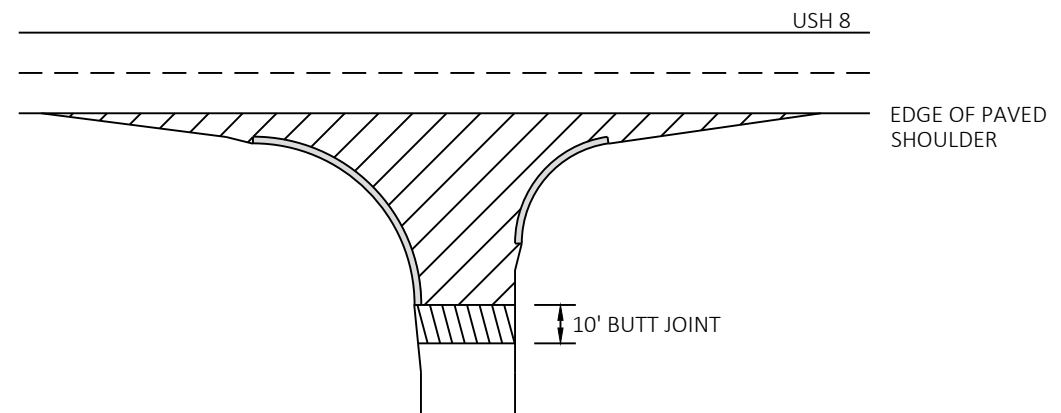
FINISHED TYPICAL SECTION FOR USH 8
 STA 329+00 - STA 402+80



PAVEMENT LAYERS	
** 1.75" HMA PAVEMENT	1.75" (4 MT 58-34 S)



FINISHED TYPICAL SECTION FOR SIDE ROADS

- * RIVER BEND ROAD - 11'
- PELICAN RANCH ROAD - 11'
- CTH P - 12'



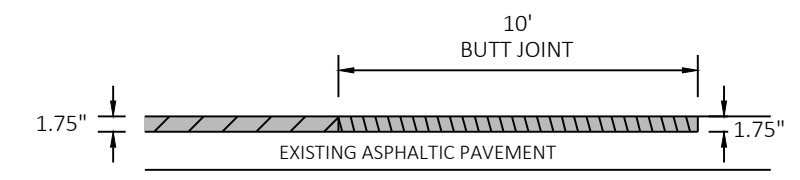
-  REMOVING ASPHALTIC SURFACE MILLING
-  REMOVING ASPHALTIC SURFACE BUTT JOINTS SEE BUTT JOINT DETAIL




NOTE: WHEN MATCHING TO AN UNPAVED SURFACE BUTT JOINT IS NOT REQUIRED

SIDE ROADS

WITH CURB AND GUTTER

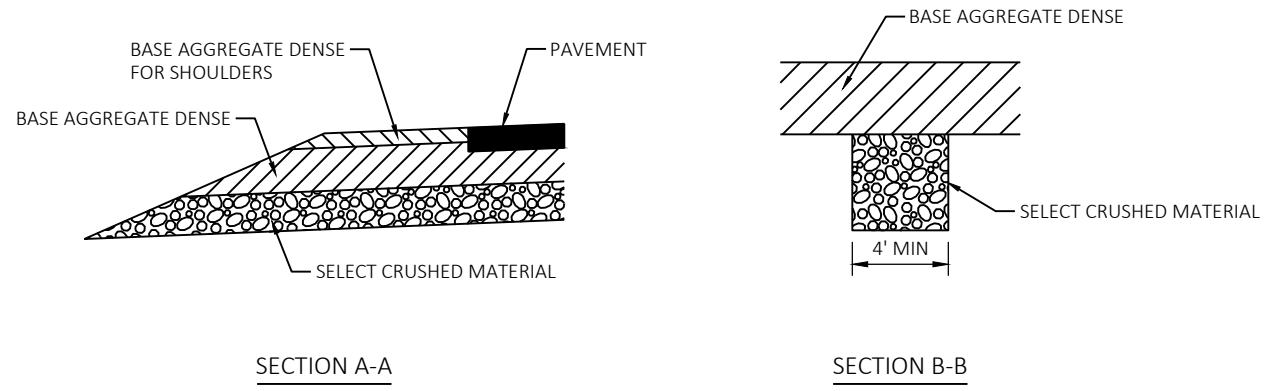
- RIVER BEND ROAD
- PELICAN RANCH ROAD
- CTH P



-  HMA PAVEMENT
-  REMOVING ASPHALTIC SURFACE MILLING
-  REMOVING ASPHALTIC SURFACE BUTT JOINTS

BUTT JOINT

MAINLINE AND SIDE ROADS

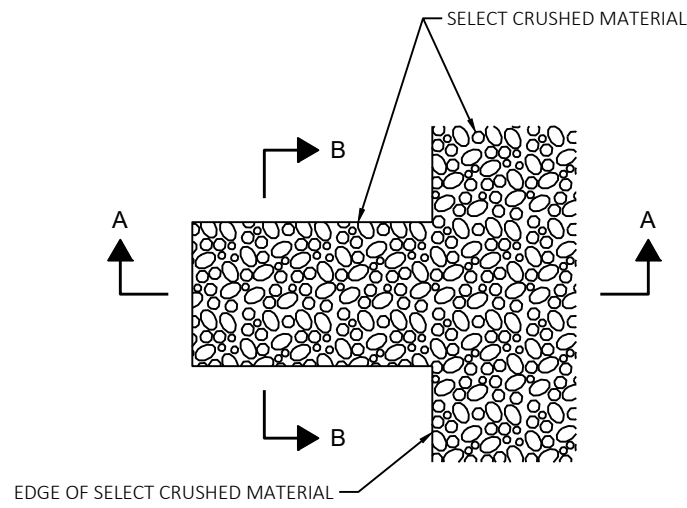


NOTES:

BASE COURSE BELOW PROPOSED CURB & GUTTER SHALL BE CONSTRUCTED TO PROVIDE A SUITABLE BASE AS DETERMINED BY THE ENGINEER IN THE FIELD

SEE MISCELLANEOUS QUANTITIES FOR FINISHING AND EROSION CONTROL ITEMS

SEE SPECIFIC CURB RAMP DETAILS FOR FURTHER INFORMATION

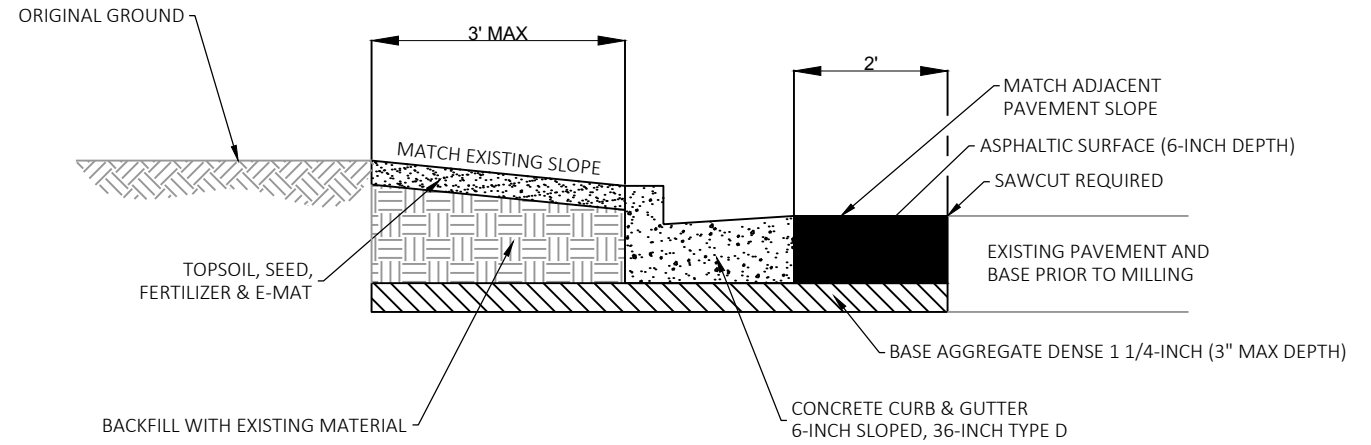


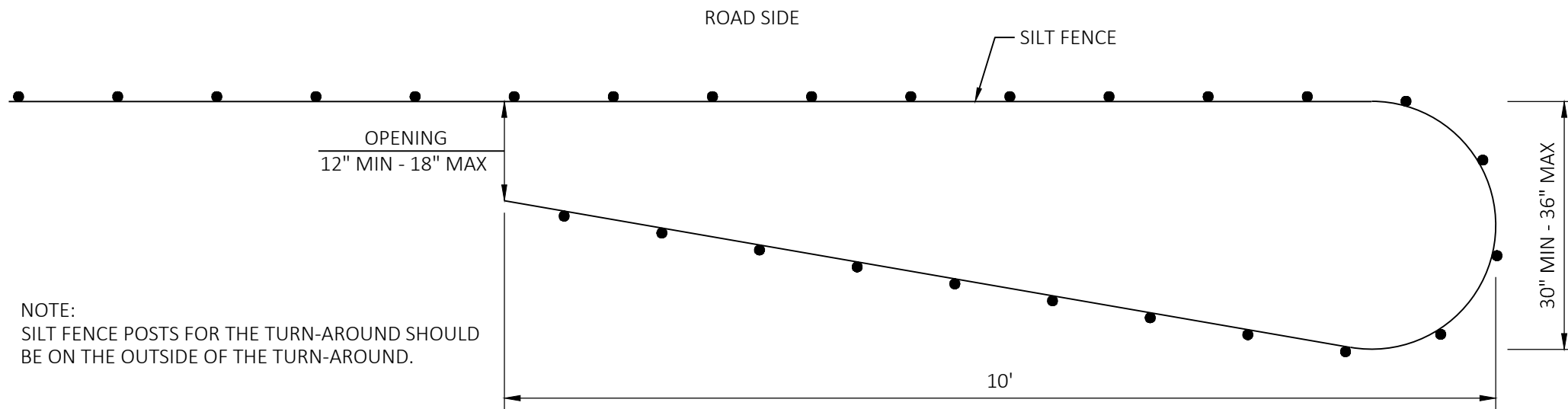
DETAIL FOR FRENCH DRAINS

DRAINS ARE TO BE CONSTRUCTED AT LEAST EVERY 250' AND AT EACH SAG VERTICAL CURVE IN THE PROFILE.

LOCATIONS TO BE DETERMINED IN THE FIELD BY THE ENGINEER.

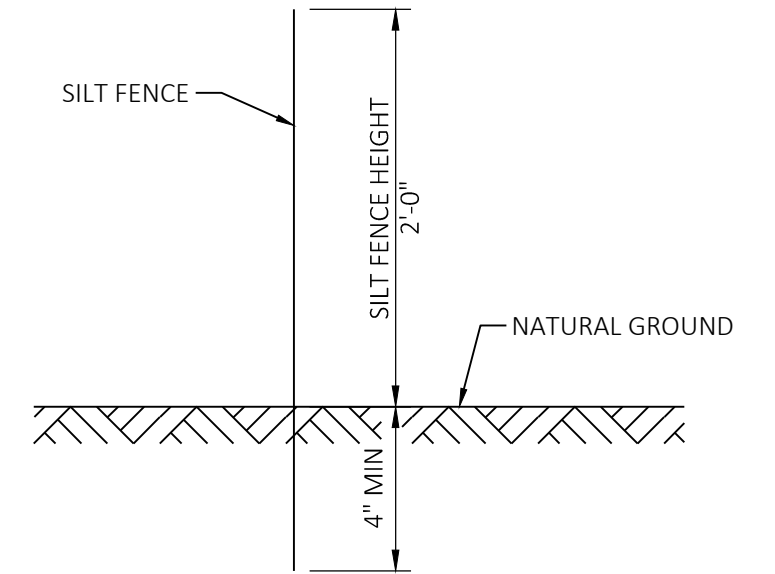
EXCAVATION REQUIRED TO CONSTRUCT FRENCH DRAINS SHALL BE CONSIDERED INCIDENTAL TO THE ITEM SELECT CRUSH MATERIAL.





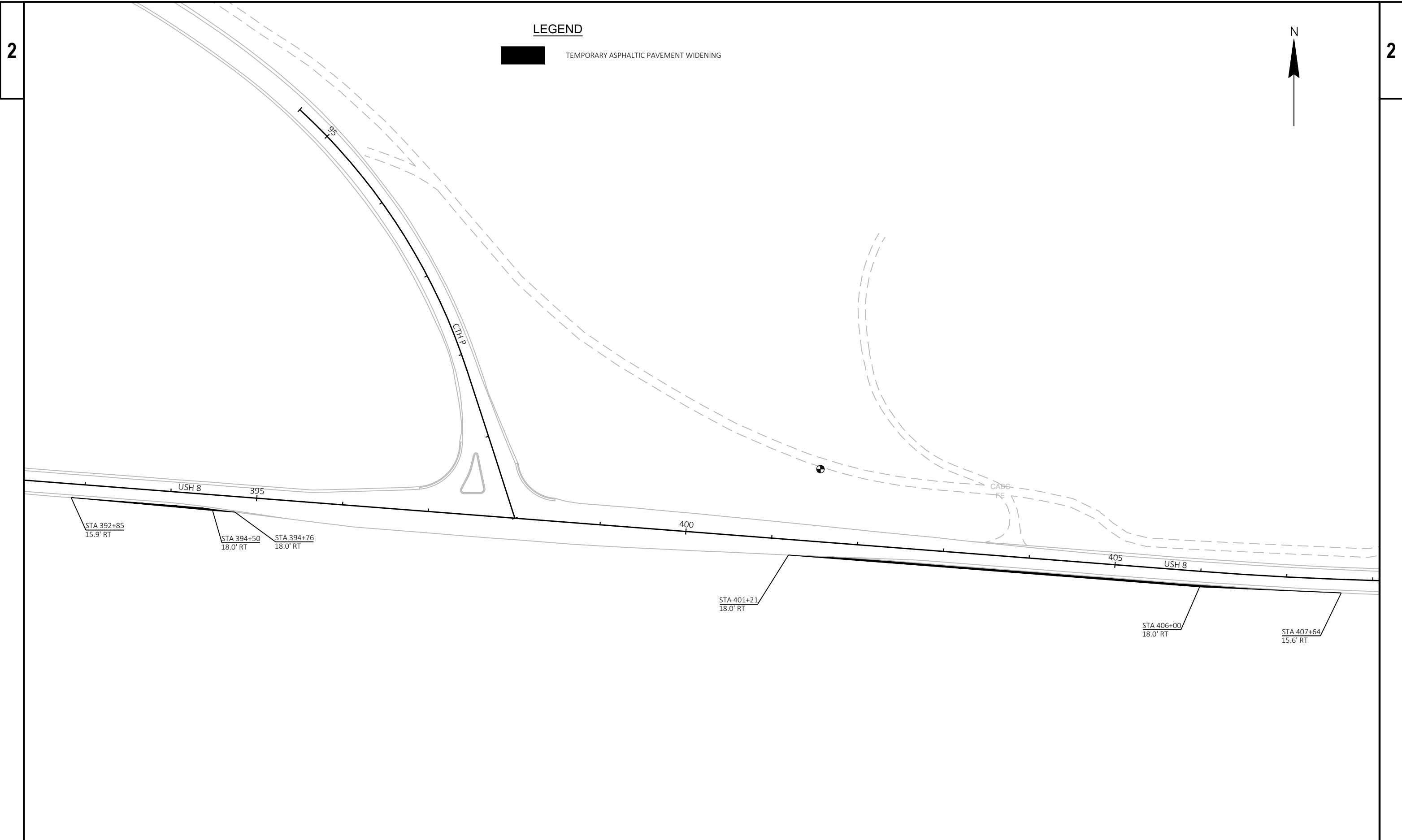
NOTE:
SILT FENCE POSTS FOR THE TURN-AROUND SHOULD
BE ON THE OUTSIDE OF THE TURN-AROUND.

PLAN VIEW



SIDE VIEW

SILT FENCE TURN-AROUND DETAIL



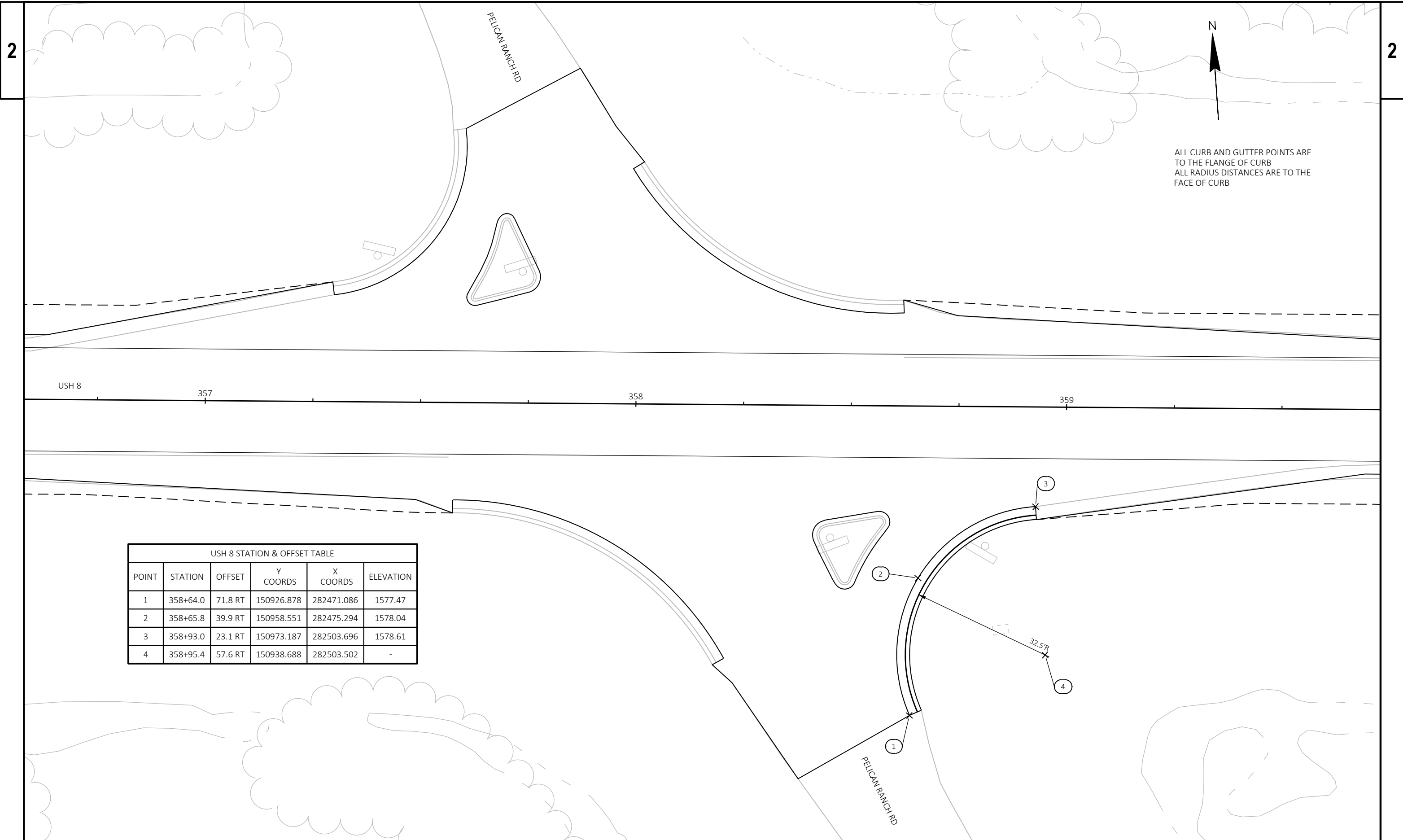
2

2

LEGEND

TEMPORARY ASPHALTIC PAVEMENT WIDENING





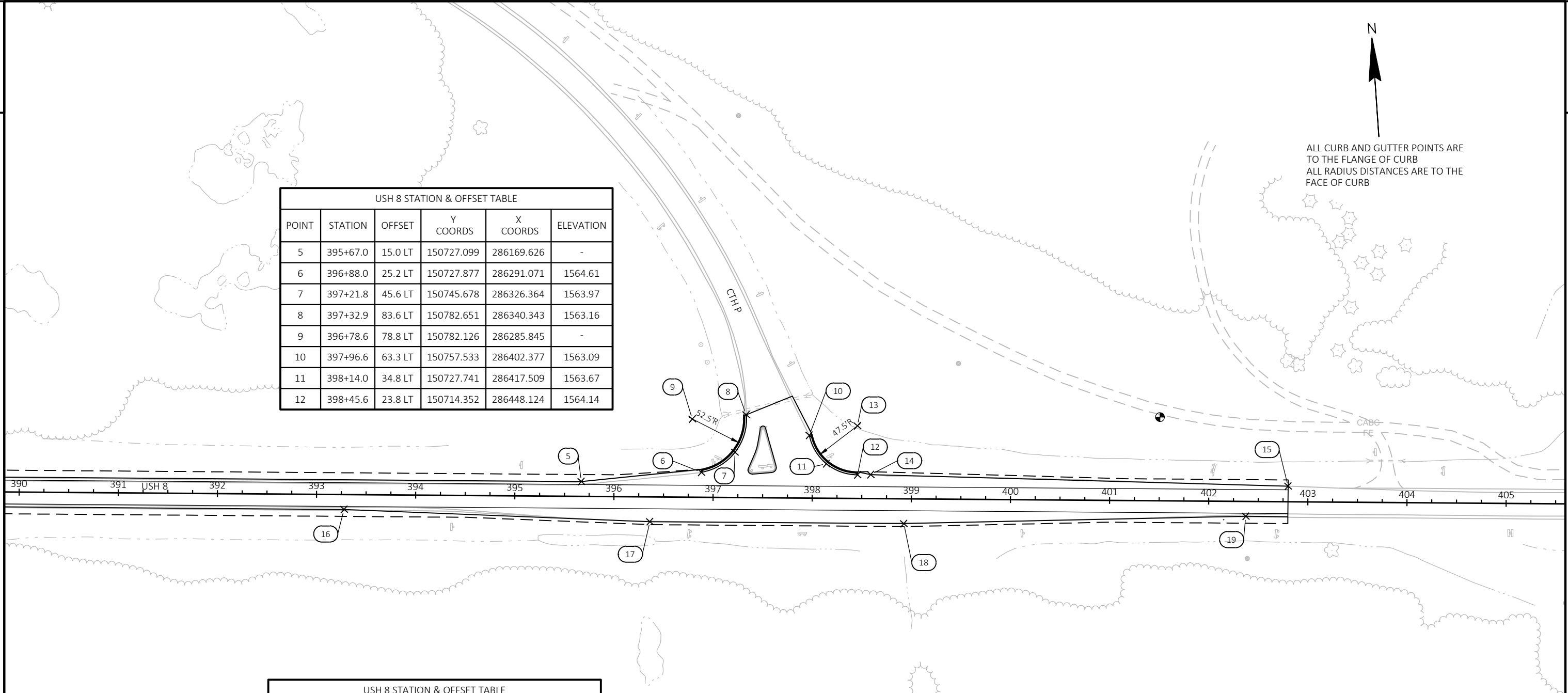
ALL CURB AND GUTTER POINTS ARE TO THE FLANGE OF CURB
 ALL RADIUS DISTANCES ARE TO THE FACE OF CURB

USH 8 STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
1	358+64.0	71.8 RT	150926.878	282471.086	1577.47
2	358+65.8	39.9 RT	150958.551	282475.294	1578.04
3	358+93.0	23.1 RT	150973.187	282503.696	1578.61
4	358+95.4	57.6 RT	150938.688	282503.502	-



ALL CURB AND GUTTER POINTS ARE TO THE FLANGE OF CURB
ALL RADIUS DISTANCES ARE TO THE FACE OF CURB

USH 8 STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
5	395+67.0	15.0 LT	150727.099	286169.626	-
6	396+88.0	25.2 LT	150727.877	286291.071	1564.61
7	397+21.8	45.6 LT	150745.678	286326.364	1563.97
8	397+32.9	83.6 LT	150782.651	286340.343	1563.16
9	396+78.6	78.8 LT	150782.126	286285.845	-
10	397+96.6	63.3 LT	150757.533	286402.377	1563.09
11	398+14.0	34.8 LT	150727.741	286417.509	1563.67
12	398+45.6	23.8 LT	150714.352	286448.124	1564.14



USH 8 STATION & OFFSET TABLE					
POINT	STATION	OFFSET	Y COORDS	X COORDS	ELEVATION
13	398+45.1	73.3 LT	150763.738	286451.486	-
14	398+58.9	24.2 LT	150713.660	286461.374	-
15	402+80.0	15.7 LT	150672.683	286880.595	-
16	393+28.0	15.0 RT	150715.672	285929.015	-
17	396+36.5	25.0 RT	150681.841	286235.859	-
18	398+92.6	25.0 RT	150662.033	286491.234	-
19	402+37.5	15.0 RT	150645.332	286835.862	-

LEGEND

- ▣▣▣▣ EROSION MAT URBAN CLASS I, TYPE B
- SILT FENCE



PELICAN RIVER

EXCLUSIONARY FENCING FOR WOOD TURTLES

EXCLUSIONARY FENCING FOR WOOD TURTLES

EXCLUSIONARY FENCING FOR WOOD TURTLES

EXCLUSIONARY FENCING FOR WOOD TURTLES

STRUCTURE B-43-24 TO REMAIN

304 USH 8 305 306 307 308 309 310

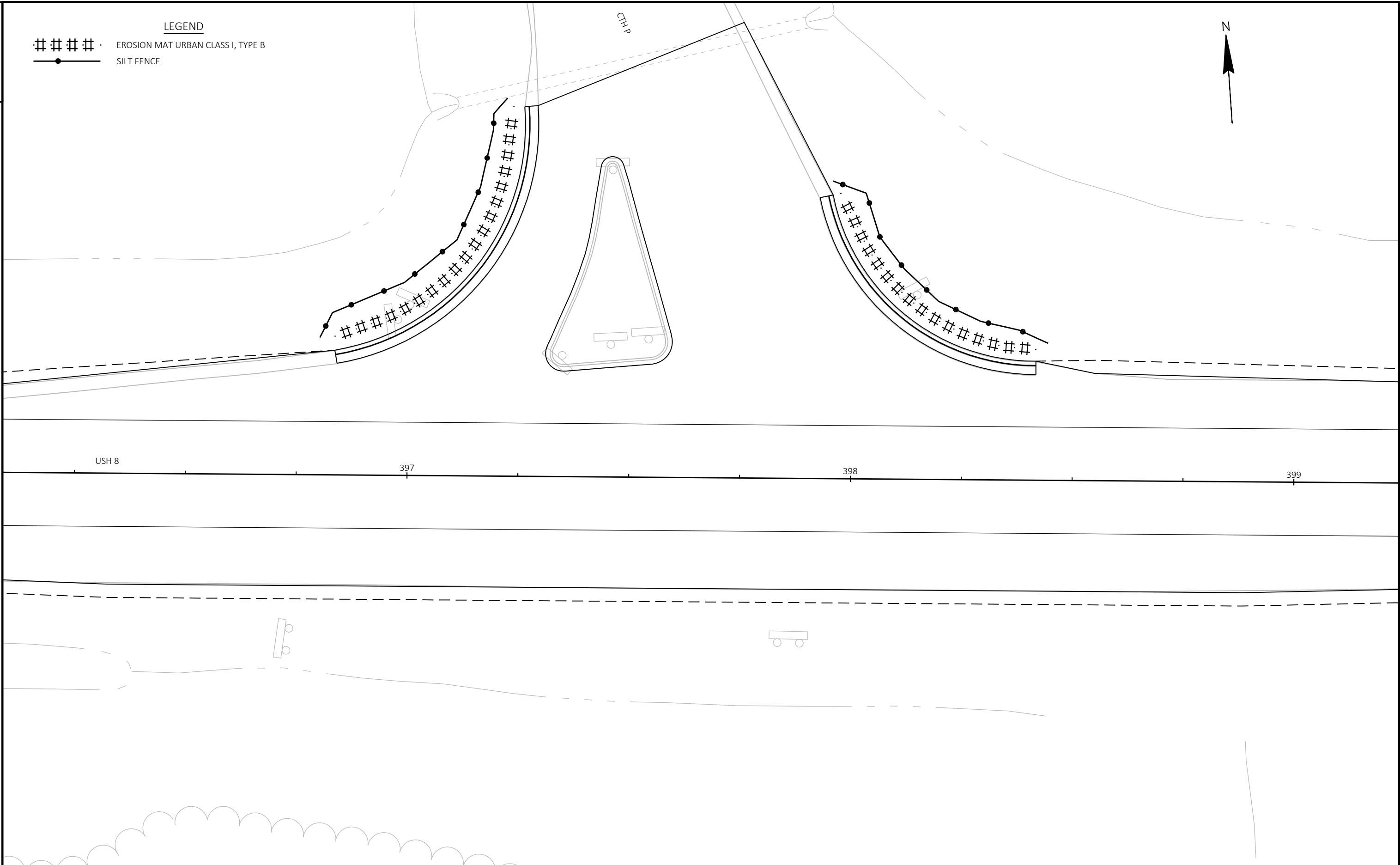
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2

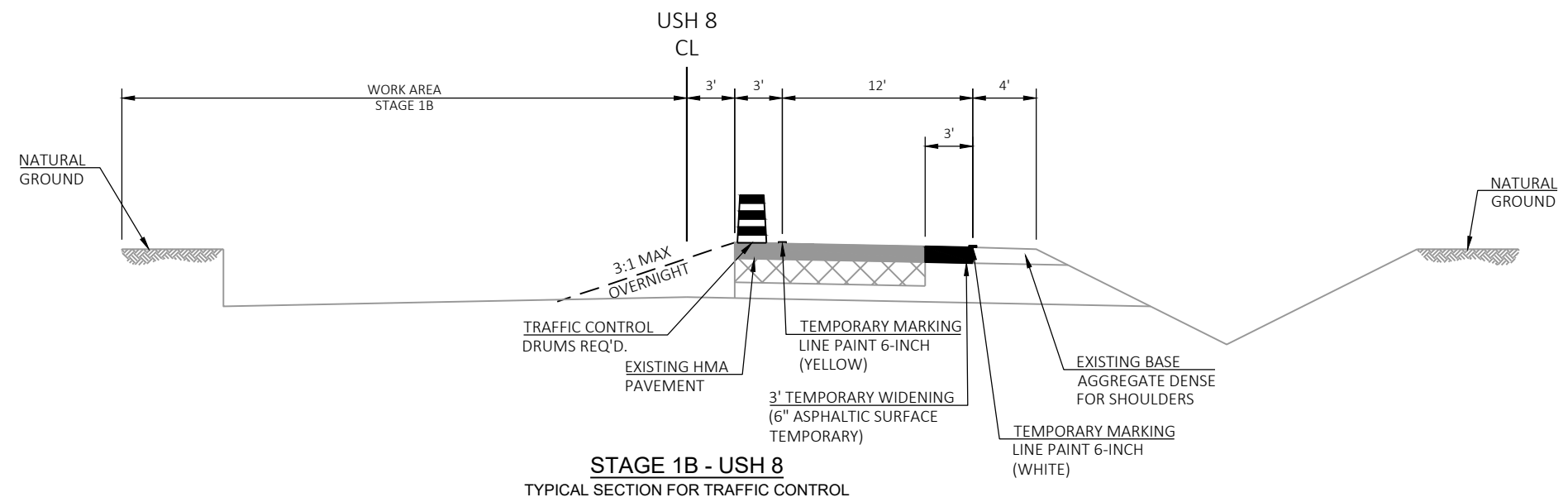
LEGEND

- ##### EROSION MAT URBAN CLASS I, TYPE B
- SILT FENCE









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PROJECT NO: 1590-12-76	HWY: USH 8	COUNTY: ONEIDA	EROSION CONTROL	SHEET	E
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LEGEND

-  TYPE III BARRICADE
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
-  REMOVING PAVEMENT MARKING
-  TEMPORARY ASPHALTIC PAVEMENT WIDENING
-  WORK AREA

CONSTRUCTION STAGE 1 NOTES:

STAGE 1A TRAFFIC

- PAVE TEMPORARY WIDENING FROM STA 392+85 TO STA 407+15.
- SEE SDD "TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY" FOR ADDITIONAL INFORMATION.

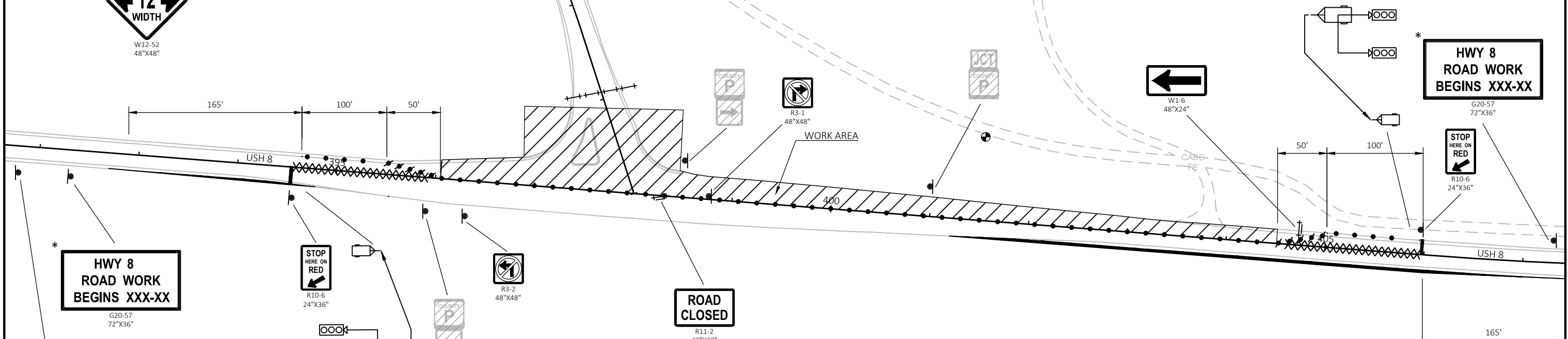
STAGE 1B TRAFFIC

- SEE SDD "TRAFFIC CONTROL, ONE LANE ROAD WITH TEMPORARY SIGNALS" FOR ADDITIONAL INFORMATION.
- SEE SDD "BRIDGE TEMPORARY TRAFFIC SIGNAL INSTALLATION" FOR ADDITIONAL INFORMATION.
- ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED.



WIDTH RESTRICTION SIGNS TO BE PLACED AT STH 17 NORTH, 0.18 MILES WEST OF THE PROJECT LIMITS.

SEE SDD "BARRICADES AND SIGNS FOR MAINLINE, DETOUR, ON RAMP, OFF RAMP CLOSURES AND ADVANCED WIDTH"; SHEET F "ADVANCED WIDTH RESTRICTION SIGNING" FOR ADDITIONAL INFORMATION.



HWY 8 ROAD WORK BEGINS XXX-XX
G20-57
72"X36"

ROAD CLOSED
R11-2
48"X30"

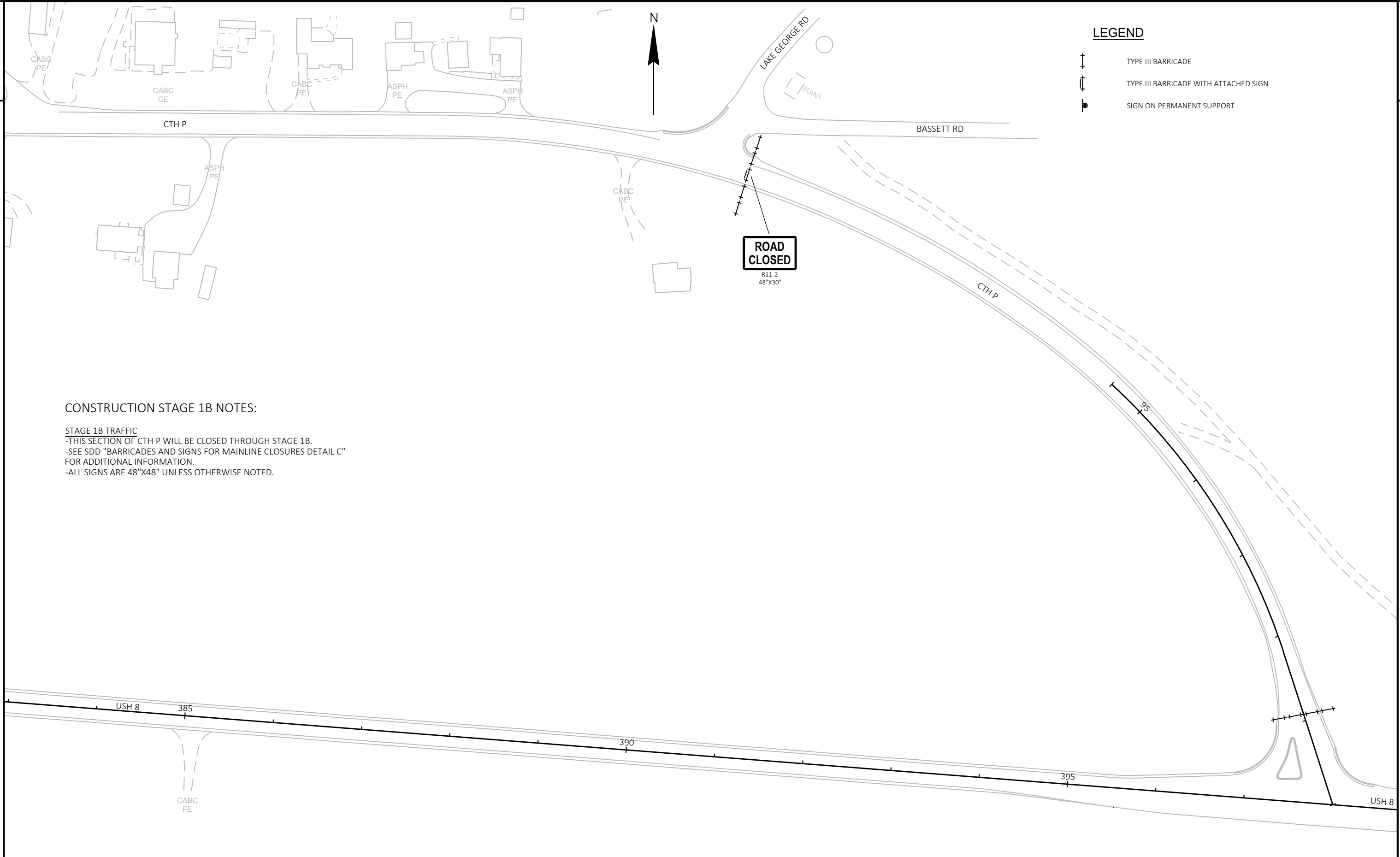
HWY 8 ROAD WORK BEGINS XXX-XX
G20-57
72"X36"



WIDTH RESTRICTION SIGNS TO BE PLACED AT USH 45, 8 MILES EAST OF THE PROJECT LIMITS.

SEE SDD "BARRICADES AND SIGNS FOR MAINLINE, DETOUR, ON RAMP, OFF RAMP CLOSURES AND ADVANCED WIDTH"; SHEET F "ADVANCED WIDTH RESTRICTION SIGNING" FOR ADDITIONAL INFORMATION.

* PLACE G20-57 SIGNS (72"X36") AT THE TERMINI OF THE STAGE 1A 7 CALENDAR DAYS PRIOR TO CONSTRUCTION AND REMOVE WHEN CONSTRUCTION BEGINS.



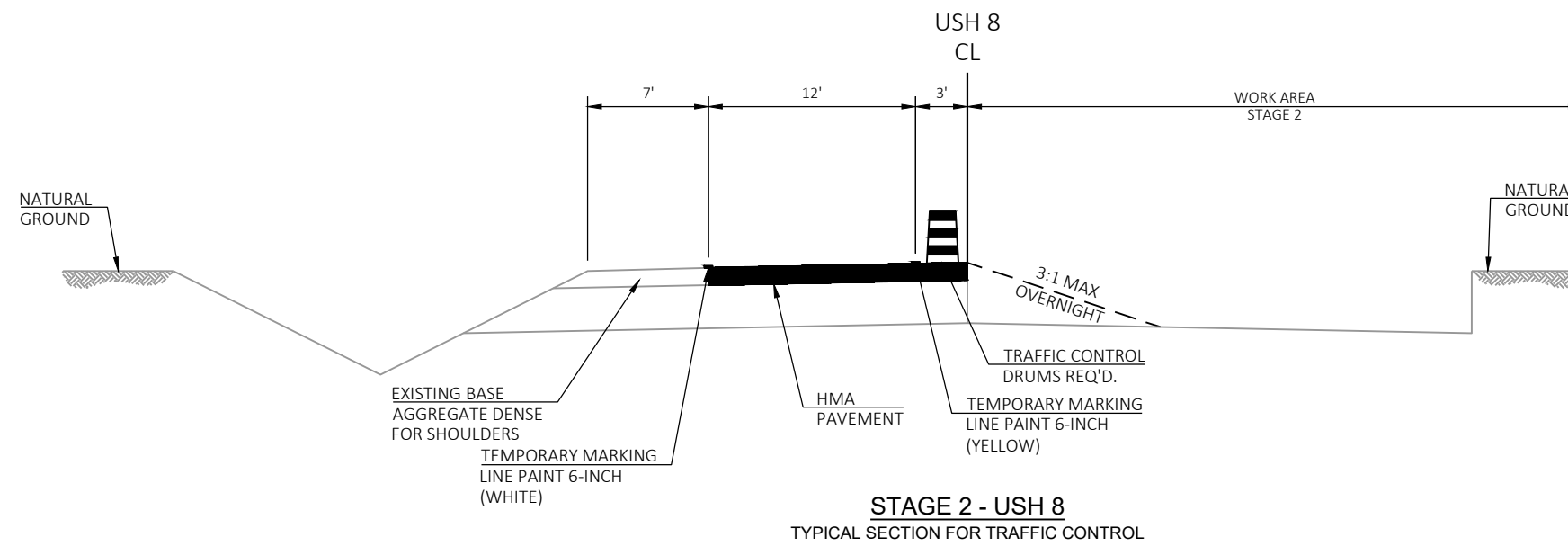
LEGEND

- ⊥ TYPE III BARRICADE
- ⊥ TYPE III BARRICADE WITH ATTACHED SIGN
- SIGN ON PERMANENT SUPPORT







CONSTRUCTION STAGE 1B NOTES:

STAGE 1B TRAFFIC
 -THIS SECTION OF CTH P WILL BE CLOSED THROUGH STAGE 1B.
 -SEE SDD "BARRICADES AND SIGNS FOR MAINLINE CLOSURES DETAIL C"
 FOR ADDITIONAL INFORMATION.
 -ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED.

PROJECT NO: 1590-12-76	HWY: USH 8	COUNTY: ONEIDA	TRAFFIC CONTROL - STAGE 1B	SHEET	E
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LEGEND

-  TYPE III BARRICADE WITH ATTACHED SIGN
-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
-  REMOVING PAVEMENT MARKING
-  WORK AREA

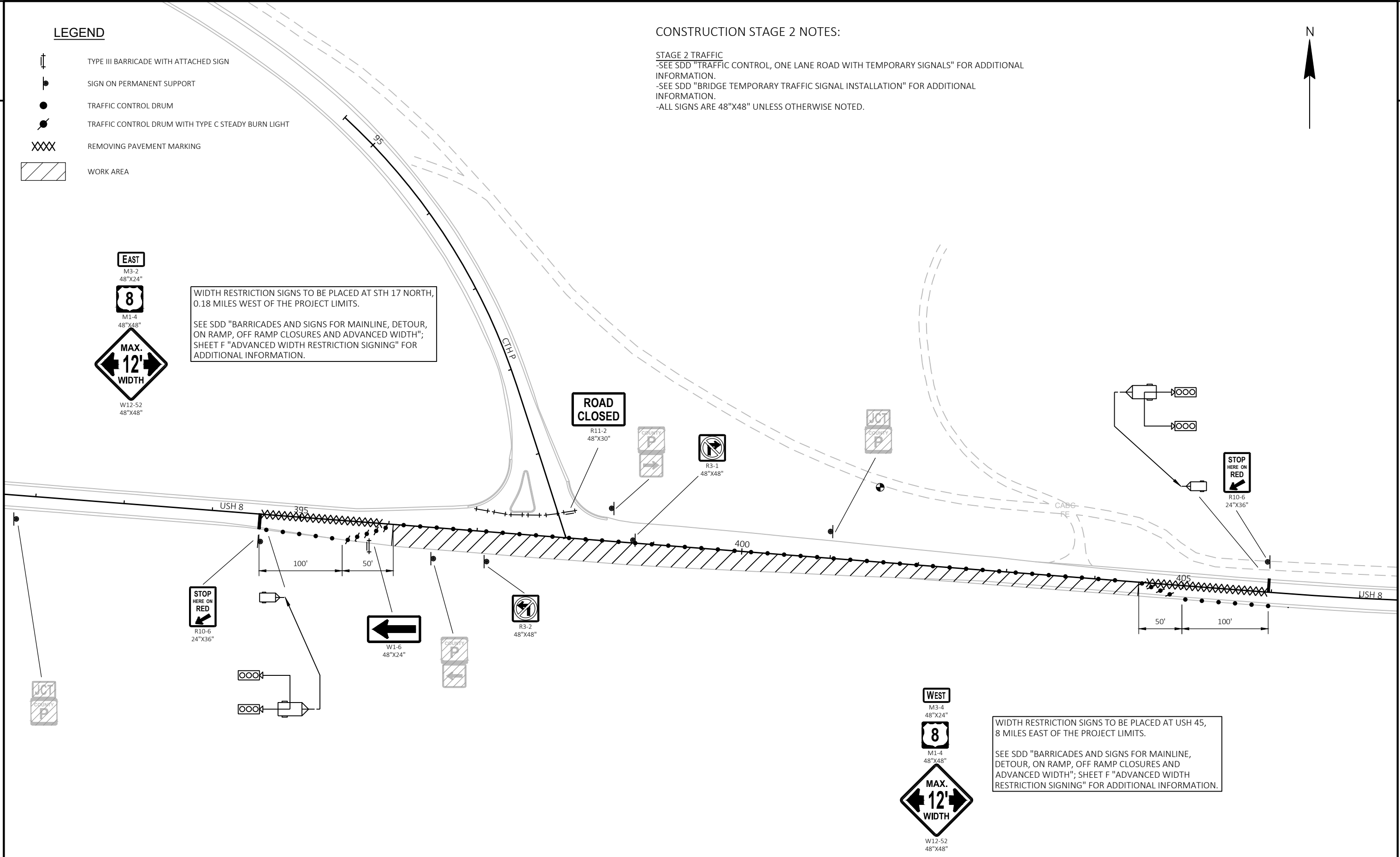
CONSTRUCTION STAGE 2 NOTES:

STAGE 2 TRAFFIC
 -SEE SDD "TRAFFIC CONTROL, ONE LANE ROAD WITH TEMPORARY SIGNALS" FOR ADDITIONAL INFORMATION.
 -SEE SDD "BRIDGE TEMPORARY TRAFFIC SIGNAL INSTALLATION" FOR ADDITIONAL INFORMATION.
 -ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED.



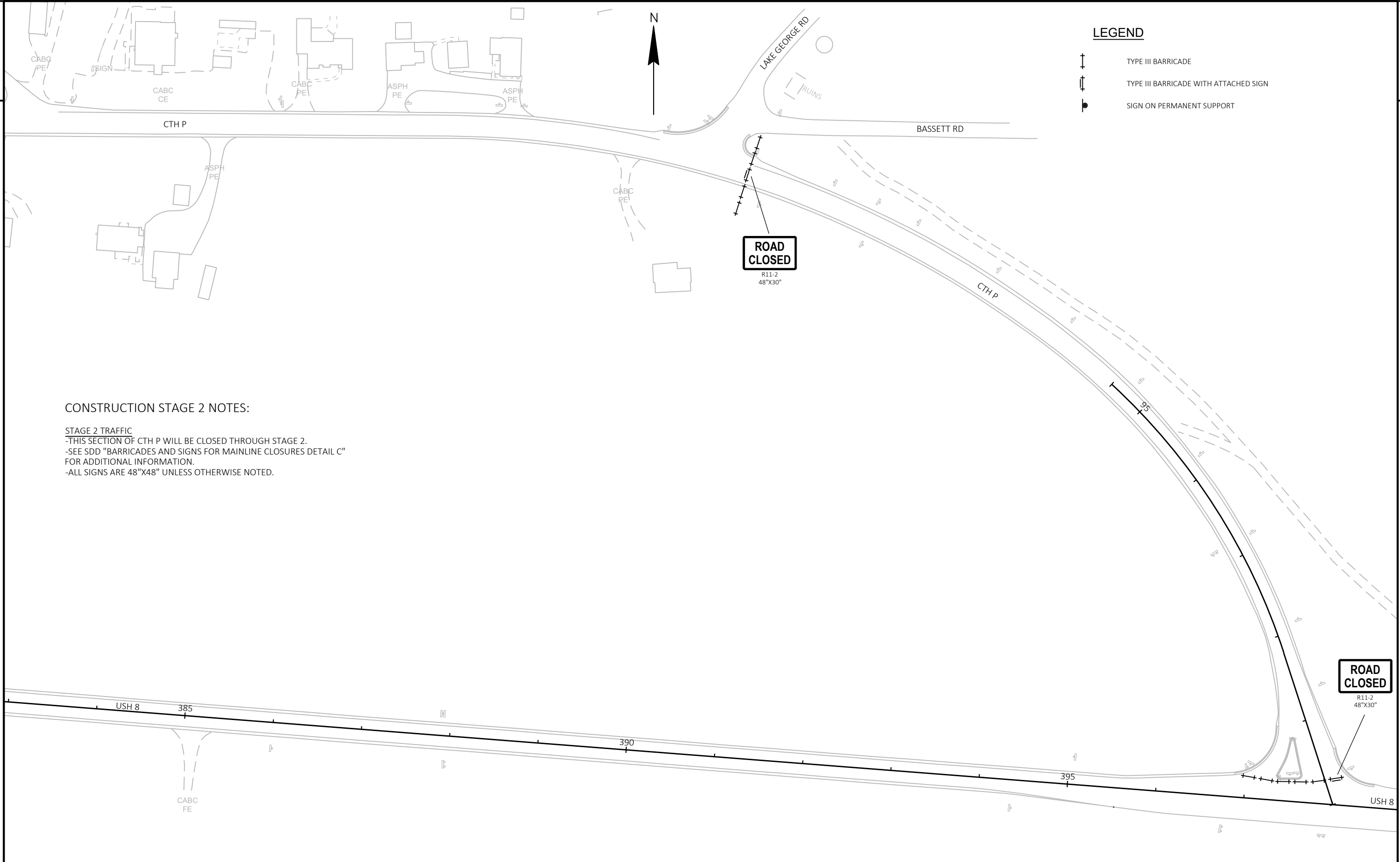
WIDTH RESTRICTION SIGNS TO BE PLACED AT STH 17 NORTH, 0.18 MILES WEST OF THE PROJECT LIMITS.

SEE SDD "BARRICADES AND SIGNS FOR MAINLINE, DETOUR, ON RAMP, OFF RAMP CLOSURES AND ADVANCED WIDTH"; SHEET F "ADVANCED WIDTH RESTRICTION SIGNING" FOR ADDITIONAL INFORMATION.



WIDTH RESTRICTION SIGNS TO BE PLACED AT USH 45, 8 MILES EAST OF THE PROJECT LIMITS.

SEE SDD "BARRICADES AND SIGNS FOR MAINLINE, DETOUR, ON RAMP, OFF RAMP CLOSURES AND ADVANCED WIDTH"; SHEET F "ADVANCED WIDTH RESTRICTION SIGNING" FOR ADDITIONAL INFORMATION.



CONSTRUCTION STAGE 2 NOTES:

- STAGE 2 TRAFFIC
- THIS SECTION OF CTH P WILL BE CLOSED THROUGH STAGE 2.
- SEE SDD "BARRICADES AND SIGNS FOR MAINLINE CLOSURES DETAIL C" FOR ADDITIONAL INFORMATION.
- ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED.

LEGEND





- ⊥ TYPE III BARRICADE
- ⊥ TYPE III BARRICADE WITH ATTACHED SIGN
- SIGN ON PERMANENT SUPPORT

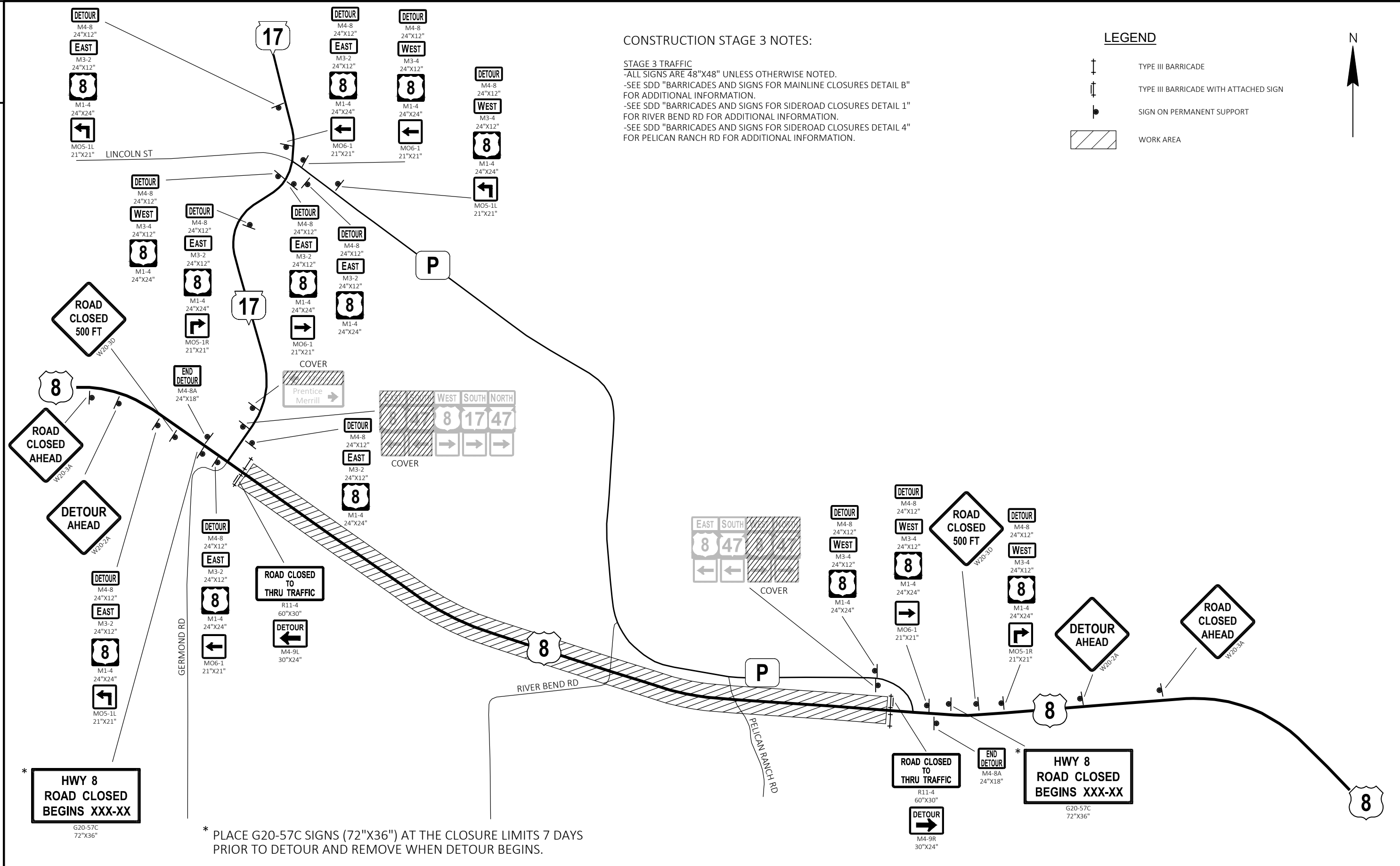
PROJECT NO: 1590-12-76	HWY: USH 8	COUNTY: ONEIDA	TRAFFIC CONTROL - STAGE 2	SHEET	E
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CONSTRUCTION STAGE 3 NOTES:

STAGE 3 TRAFFIC
 -ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED.
 -SEE SDD "BARRICADES AND SIGNS FOR MAINLINE CLOSURES DETAIL B" FOR ADDITIONAL INFORMATION.
 -SEE SDD "BARRICADES AND SIGNS FOR SIDEROAD CLOSURES DETAIL 1" FOR RIVER BEND RD FOR ADDITIONAL INFORMATION.
 -SEE SDD "BARRICADES AND SIGNS FOR SIDEROAD CLOSURES DETAIL 4" FOR PELICAN RANCH RD FOR ADDITIONAL INFORMATION.

LEGEND

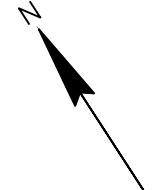
-  TYPE III BARRICADE
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  SIGN ON PERMANENT SUPPORT
-  WORK AREA



* PLACE G20-57C SIGNS (72"X36") AT THE CLOSURE LIMITS 7 DAYS PRIOR TO DETOUR AND REMOVE WHEN DETOUR BEGINS.

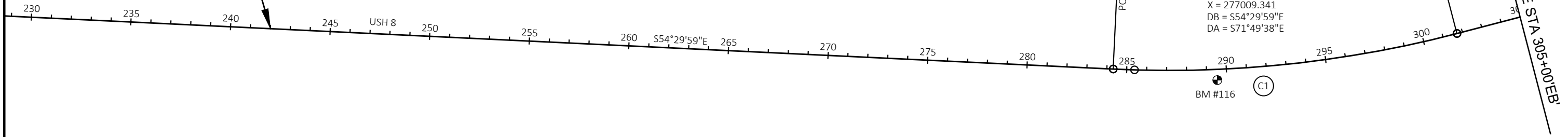
USH 8 CURVE DATA

(C1)
 PI STA = 293+08.8
 Y = 152784.837
 X = 276176.596
 DELTA = 17°19'39" LT
 D = 0°59'46"
 T = 876.5'
 L = 1739.5'
 R = 5752.0'
 PC STA = 284+32.4
 Y = 153293.805
 X = 275463.058
 PT STA = 301+71.9
 Y = 152511.483
 X = 277009.341
 DB = S54°29'59"E
 DA = S71°49'38"E



BEGIN PROJECT

STA 242+00
 Y = 155751.577
 X = 272017.433



BENCH MARKS			
NO.	STATION	ELEV.	DESCRIPTION
116	289+48	1537.20	FENO MONUMENT, 54' RT

RIVER BEND ROAD CURVE DATA

(C2)
 PI STA = 72+55.0RB
 Y = 151332.798
 X = 279543.885
 DELTA = 41°19'58" LT
 D = 24°39'44"
 T = 87.6'
 L = 167.6'
 R = 232.3'
 PC STA = 71+67.3RB
 Y = 151324.436
 X = 279456.654
 PT STA = 73+34.9RB
 Y = 151396.687
 X = 279603.863
 DB = N84°31'28"E
 DA = N43°11'31"E

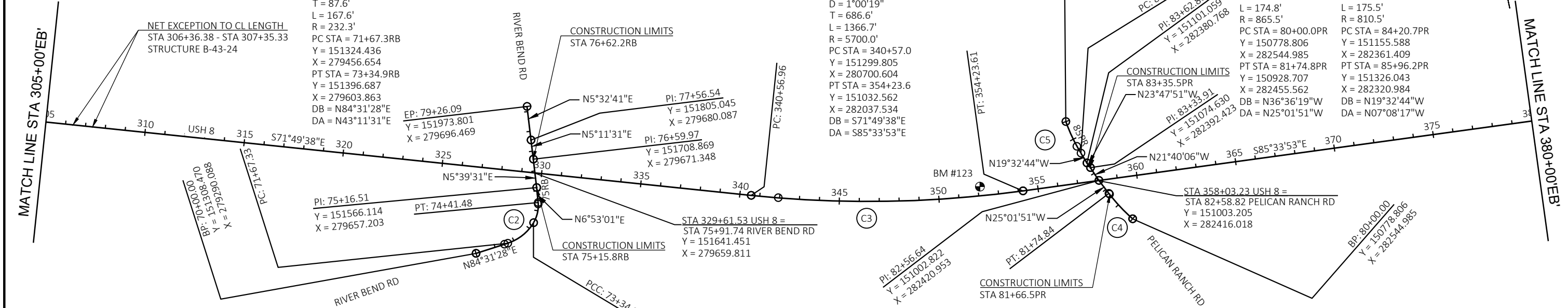
USH 8 CURVE DATA

(C3)
 PI STA = 347+43.6
 Y = 151085.661
 X = 281352.973
 DELTA = 13°44'15" LT
 D = 1°00'19"
 T = 686.6'
 L = 1366.7'
 R = 5700.0'
 PC STA = 340+57.0
 Y = 151299.805
 X = 280700.604
 PT STA = 354+23.6
 Y = 151032.562
 X = 282037.534
 DB = S71°49'38"E
 DA = S85°33'53"E

PELICAN RANCH ROAD CURVE DATA

(C4)
 PI STA = 80+87.7PR
 Y = 150849.225
 X = 282492.678
 DELTA = 11°34'28" RT
 D = 6°37'11"
 T = 87.7'
 L = 174.8'
 R = 865.5'
 PC STA = 80+00.0PR
 Y = 150778.806
 X = 282544.985
 PT STA = 81+74.8PR
 Y = 150928.707
 X = 282455.562
 DB = N36°36'19"W
 DA = N25°01'51"W

(C5)
 PI STA = 85+08.8PR
 Y = 151238.618
 X = 282331.932
 DELTA = 12°24'27" RT
 D = 7°04'08"
 T = 88.1'
 L = 175.5'
 R = 810.5'
 PC STA = 84+20.7PR
 Y = 151155.588
 X = 282361.409
 PT STA = 85+96.2PR
 Y = 151326.043
 X = 282320.984
 DB = N19°32'44"W
 DA = N07°08'17"W



BENCH MARKS			
NO.	STATION	ELEV.	DESCRIPTION
123	352+06	1579.92	FENO MONUMENT, 46' LT



USH 8 CURVE DATA

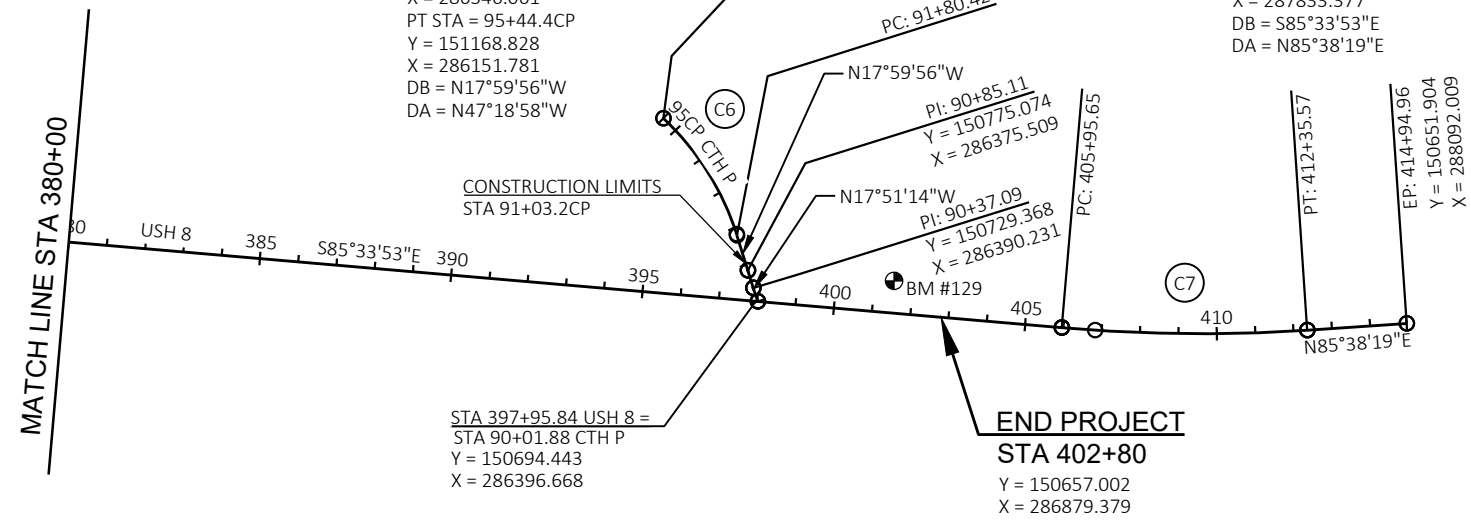
(C7)

PI STA = 409+16.2
 Y = 150607.799
 X = 287513.717
 DELTA = 8°47'48" LT
 D = 1°22'29"
 T = 320.6'
 L = 639.9'
 R = 4168.0'
 PC STA = 405+95.7
 Y = 150632.591
 X = 287194.088
 PT STA = 412+35.6
 Y = 150632.179
 X = 287833.377
 DB = S85°33'53"E
 DA = N85°38'19"E

CTH P CURVE DATA

(C6)

PI STA = 93+66.5CP
 Y = 151042.680
 X = 286288.564
 DELTA = 29°19'02" LT
 D = 8°03'16"
 T = 186.1'
 L = 364.0'
 R = 711.4'
 PC STA = 91+80.4CP
 Y = 150865.713
 X = 286346.061
 PT STA = 95+44.4CP
 Y = 151168.828
 X = 286151.781
 DB = N17°59'56"W
 DA = N47°18'58"W



BENCH MARKS

NO.	STATION	ELEV.	DESCRIPTION
129	401+45	1566.94	FENO MONUMENT, 84' LT

Estimate Of Quantities

1590-12-76

Line	Item	Item Description	Unit	Total	Qty
0002	204.0100	Removing Concrete Pavement	SY	54,514.000	54,514.000
0004	204.0115	Removing Asphaltic Surface Butt Joints	SY	202.000	202.000
0006	204.0120	Removing Asphaltic Surface Milling	SY	2,944.000	2,944.000
0008	204.0150	Removing Curb & Gutter	LF	238.000	238.000
0010	205.0100	Excavation Common	CY	63,649.000	63,649.000
0012	213.0100	Finishing Roadway (project) 01. 1590-12-76	EACH	1.000	1.000
0014	305.0110	Base Aggregate Dense 3/4-Inch	TON	9,692.000	9,692.000
0016	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	48,751.000	48,751.000
0018	312.0110	Select Crushed Material	TON	38,950.000	38,950.000
0020	371.2000.S	QMP Base Aggregate Dense 1 1/4-Inch Compaction	EACH	23.000	23.000
0022	415.0410	Concrete Pavement Approach Slab	SY	104.000	104.000
0024	450.4000	HMA Cold Weather Paving	TON	1,846.000	1,846.000
0026	455.0605	Tack Coat	GAL	7,964.000	7,964.000
0028	460.2000	Incentive Density HMA Pavement	DOL	11,820.000	11,820.000
0030	460.6223	HMA Pavement 3 MT 58-28 S	TON	12,731.000	12,731.000
0032	460.6244	HMA Pavement 4 MT 58-34 S	TON	5,729.000	5,729.000
0034	465.0110	Asphaltic Surface Patching	TON	170.000	170.000
0036	465.0125	Asphaltic Surface Temporary	TON	58.000	58.000
0038	465.0560	Asphaltic Rumble Strips, Centerline	LF	16,045.000	16,045.000
0040	601.0557	Concrete Curb & Gutter 6-Inch Sloped 36-Inch Type D	LF	238.000	238.000
0042	618.0100	Maintenance and Repair of Haul Roads (project) 01. 1590-12-76	EACH	1.000	1.000
0044	619.1000	Mobilization	EACH	1.000	1.000
0046	624.0100	Water	MGAL	880.000	880.000
0048	625.0100	Topsoil	SY	66.000	66.000
0050	628.1504	Silt Fence	LF	1,230.000	1,230.000
0052	628.1520	Silt Fence Maintenance	LF	1,230.000	1,230.000
0054	628.1905	Mobilizations Erosion Control	EACH	4.000	4.000
0056	628.1910	Mobilizations Emergency Erosion Control	EACH	3.000	3.000
0058	628.2008	Erosion Mat Urban Class I Type B	SY	66.000	66.000
0060	628.7504	Temporary Ditch Checks	LF	50.000	50.000
0062	629.0210	Fertilizer Type B	CWT	7.600	7.600
0064	630.0130	Seeding Mixture No. 30	LB	305.000	305.000
0066	642.5001	Field Office Type B	EACH	1.000	1.000
0068	643.0300	Traffic Control Drums	DAY	603.000	603.000
0070	643.0420	Traffic Control Barricades Type III	DAY	2,520.000	2,520.000
0072	643.0705	Traffic Control Warning Lights Type A	DAY	3,580.000	3,580.000
0074	643.0715	Traffic Control Warning Lights Type C	DAY	300.000	300.000
0076	643.0900	Traffic Control Signs	DAY	9,985.000	9,985.000
0078	643.0920	Traffic Control Covering Signs Type II	EACH	11.000	11.000
0080	643.1000	Traffic Control Signs Fixed Message	SF	72.000	72.000
0082	643.3165	Temporary Marking Line Paint 6-Inch	LF	3,400.000	3,400.000
0084	643.3850	Temporary Marking Stop Line Removable Tape 18-Inch	LF	32.000	32.000
0086	643.5000	Traffic Control	EACH	1.000	1.000
0088	646.2040	Marking Line Grooved Wet Ref Epoxy 6-Inch	LF	46,961.000	46,961.000
0090	646.4020	Marking Line Epoxy 10-Inch	LF	157.000	157.000
0092	646.5020	Marking Arrow Epoxy	EACH	1.000	1.000
0094	646.5120	Marking Word Epoxy	EACH	1.000	1.000
0096	646.6120	Marking Stop Line Epoxy 18-Inch	LF	37.000	37.000
0098	646.6466	Cold Weather Marking Epoxy 6-Inch	LF	4,697.000	4,697.000
0100	646.6470	Cold Weather Marking Epoxy 10-Inch	LF	16.000	16.000

Estimate Of Quantities

1590-12-76

Line	Item	Item Description	Unit	Total	Qty
0102	646.9000	Marking Removal Line 4-Inch	LF	225.000	225.000
0104	650.4500	Construction Staking Subgrade	LF	8,409.000	8,409.000
0106	650.5000	Construction Staking Base	LF	15,989.000	15,989.000
0108	650.5500	Construction Staking Curb Gutter and Curb & Gutter	LF	238.000	238.000
0110	650.8000	Construction Staking Resurfacing Reference	LF	15,989.000	15,989.000
0112	650.9911	Construction Staking Supplemental Control (project) 01. 1590-12-76	EACH	1.000	1.000
0114	661.0201	Temporary Traffic Signals for Intersections (location) 01. Station 398+00	EACH	1.000	1.000
0116	690.0150	Sawing Asphalt	LF	2,686.000	2,686.000
0118	690.0250	Sawing Concrete	LF	12.000	12.000
0120	715.0720	Incentive Compressive Strength Concrete Pavement	DOL	150.000	150.000
0122	740.0440	Incentive IRI Ride	DOL	12,108.000	12,108.000

REMOVING CONCRETE PAVEMENT

CATEGORY	STATION	TO	STATION	LOCATION	204.0100 SY	REMARKS
STAGE 1						
0010	396+00	-	402+80	USH 8 LT	1102	CTH P INTERSECTION
STAGE 2						
0010	396+00	-	402+80	USH 8 RT	1637	CTH P INTERSECTION
STAGE 3						
0010	242+00	-	306+44	USH 8	21752	MAINLINE
0010	307+27	-	327+00	USH 8	6914	MAINLINE
0010	327+00	-	332+25	USH 8	1744	RIVER BEND RD INTERSECTION
0010	332+25	-	356+00	USH 8	7884	MAINLINE
0010	356+00	-	360+00	USH 8	1329	PELICAN RANCH RD INTERSECTION
0010	360+00	-	396+00	USH 8	12152	MAINLINE
TOTAL 0010					54,514	

EARTHWORK SUMMARY

CATEGORY	STATION	TO	STATION	LOCATION	CUT (1)	SALVAGED/UNUSABLE PAVEMENT MATERIAL (2)	205.0100 EXCAVATION COMMON (3)	UNEXPANDED FILL (4)	EXPANDED FILL (4)	MASS ORDINATE +/- (5)
							CY	FACTOR 1.18		
0010	242+00	-	306+36	USH 8	33750	8939	33750	2	2	33748
0010	307+35	-	329+00	USH 8	10799	3007	10799	7	8	10791
0010	329+00	-	402+80	USH 8	19100	10250	19100	0	0	19100
TOTAL 0010							63,649			

NOTES:

- (1) SALVAGED/UNUSABLE PAVEMENT MATERIAL IS INCLUDED IN CUT.
- (2) SALVAGED/UNUSABLE PAVEMENT MATERIAL IS PAID AS REMOVING CONCRETE PAVEMENT
- (3) COMMON EXCAVATION = CUT, ITEM NUMBER 205.0100.
- (4) EXPANDED FILL FACTOR = 1.18, EXPANDED FILL = UNEXPANDED FILL * FILL FACTOR.
- (5) THE MASS ORDINATE + OR - QTY CALCULATED FOR THE STAGE. PLUX QUANTITY INDICATES AN EXCESS OF MATERIAL WITHIN THE DIVISION. MINUS INDICATES A SHORTAGE OF MATERIAL WITHIN THE DIVISION.

REMOVING ASPHALTIC SURFACE

CATEGORY	STATION	TO	STATION	OFFSET	LOCATION	204.0115 REMOVING ASPHALTIC SURFACE BUTT JOINTS SY	204.0120 REMOVING ASPHALTIC SURFACE MILLING SY	REMARKS
STAGE 1								
0010	395+68	-	402+80	LT	USH 8	59	978	CTH P
STAGE 3								
0010	327+00	-	331+54	RT	USH 8	35	504	RIVER BEND RD S
0010	328+28	-	332+13	LT	USH 8	36	477	RIVER BEND RD N
0010	355+99	-	360+00	RT	USH 8	35	540	PELICAN RANCH RD S
0010	356+63	-	360+00	LT	USH 8	37	445	PELICAN RANCH RD N
TOTAL 0010						202	2,944	

REMOVING CURB & GUTTER

CATEGORY	STATION	TO	STATION	LOCATION	204.0150 LF	REMARKS
STAGE 1						
0010	396+87	-	397+33	USH 8 LT	82	CTH P
0010	397+96	-	398+46	USH 8 LT	69	CTH P
STAGE 3						
0010	329+04	-	330+38	USH 8 RT	20	RIVER BEND RD SOUTH (SPOT REPAIRS)
0010	358+64	-	358+93	USH 8 RT	67	PELICAN RANCH RD SOUTH
TOTAL 0010					238	

BASE AGGREGATE DENSE

CATEGORY	STATION	TO	STATION	LOCATION	305.0110	305.0120	312.0110	624.0100	REMARKS
					BASE AGGREGATE DENSE 3/4-INCH	BASE AGGREGATE DENSE 1 1/4-INCH	SELECT CRUSHED MATERIAL	WATER MGAL	
STAGE 1									
0010	396+00	-	402+80	USH 8 LT	1058	365	-	16	CTH P INTERSECTION
STAGE 2									
0010	396+00	-	402+80	USH 8 RT	365	938	-	20	CTH P INTERSECTION
STAGE 3									
0010	242+00	-	306+17	USH 8	-	14760	20321	222	MAINLINE
0010	242+00	-	306+17	USH 8	3444	6888	9565	155	SHOULDERS
0010	307+54	-	327+00	USH 8	-	4476	6163	68	MAINLINE
0010	307+54	-	327+00	USH 8	1044	2089	2901	47	SHOULDERS
0010	327+00	-	332+25	USH 8	47	-	-	2	RIVER BEND RD INTERSECTION
0010	327+00	-	356+00	USH 8	-	5559	-	84	MAINLINE
0010	327+00	-	356+00	USH 8	1556	2594	-	63	SHOULDERS
0010	356+00	-	360+00	USH 8	31	-	-	2	PELICAN RANCH RD INTERSECTION
0010	356+00	-	393+50	USH 8	-	7188	-	108	MAINLINE
0010	356+00	-	393+50	USH 8	2013	3355	-	82	SHOULDERS
0010	393+50	-	396+00	USH 8	134	539	-	11	MAINLINE
TOTAL 0010					9,692	48,751	38,950	880	

HMA PAVEMENT

CATEGORY	STATION	TO	STATION	LOCATION	450.4000	455.0605	460.6223	460.6244	REMARKS
					HMA COLD WEATHER PAVING	TACK COAT GAL	HMA PAVEMENT 3 MT 58-28 S	HMA PAVEMENT 4 MT 58-34 S	
STAGE 1									
0010	396+00	-	402+80	USH 8 LT	-	159	-	115	MAINLINE
0010	396+00	-	402+80	USH 8 LT	-	144	266	104	CTH P INTERSECTION
STAGE 2									
0010	396+00	-	402+80	USH 8 RT	-	229	422	165	MAINLINE
STAGE 3									
0010	242+00	-	306+17	USH 8	-	2396	4428	1722	MAINLINE
0010	242+00	-	306+17	USH 8	-	599	615	431	SHOULDERS
0010	307+54	-	327+00	USH 8	-	727	1343	523	MAINLINE
0010	307+54	-	327+00	USH 8	-	182	187	131	SHOULDERS
0010	327+00	-	332+25	USH 8	-	146	-	105	RIVER BEND RD INTERSECTION
0010	327+00	-	356+00	USH 8	-	1083	2001	779	MAINLINE
0010	327+00	-	356+00	USH 8	-	271	278	195	SHOULDERS
0010	356+00	-	360+00	USH 8	-	146	-	105	PELICAN RANCH RD INTERSECTION
0010	356+00	-	393+50	USH 8	-	1400	2588	1007	MAINLINE
0010	356+00	-	393+50	USH 8	-	350	360	252	SHOULDERS
0010	393+50	-	396+00	USH 8	-	132	243	95	MAINLINE
0010	UNDISTRIBUTED			USH 8	1846	-	-	-	
TOTAL 0010					1,846	7,964	12,731	5,729	

CONCRETE PAVEMENT APPROACH SLAB

CATEGORY	STATION	TO	STATION	LOCATION	415.0410	REMARKS
					SY	
STAGE 3						
0010	306+17	-	306+36	USH 8	52	B-43-23 W. APPROACH
0010	307+35	-	307+55	USH 8	52	B-43-23 E. APPROACH
TOTAL 0010					104	

ASPHALTIC SURFACE PATCHING

CATEGORY	STATION	TO	STATION	LOCATION	465.0110	REMARKS
					TON	
STAGE 1						
0010	396+87	-	397+33	USH 8 LT	59	CTH P
0010	397+96	-	398+46	USH 8 LT	49	CTH P
STAGE 3						
0010	329+04	-	330+38	USH 8 RT	14	RIVER BEND RD SOUTH (SPOT REPAIRS)
0010	358+64	-	358+93	USH 8 RT	48	PELICAN RANCH RD SOUTH
TOTAL 0010					170	

ASPHALTIC SURFACE TEMPORARY

CATEGORY	STATION	TO	STATION	LOCATION	465.0125	REMARKS
					TON	
STAGE 1						
0010	392+85	-	394+76	USH 8 RT	8	LANE WIDENING
0010	401+22	-	407+65	USH 8 RT	50	LANE WIDENING
TOTAL 0010					58	

ASPHALTIC RUMBLE STRIPS, CENTERLINE

CATEGORY	STATION	TO	STATION	LOCATION	LF	REMARKS
465.0560						
STAGE 3	0010	242+00	-	306+04	USH 8	6,404 MAINLINE
0010	307+59	-	329+00	USH 8	2,141	MAINLINE
0010	329+00	-	330+25	USH 8	125	RIVER BEND RD INTERSECTION
0010	330+25	-	357+50	USH 8	2,725	MAINLINE
0010	357+50	-	358+50	USH 8	100	PELICAN RANCH RD INTERSECTION
0010	358+50	-	397+00	USH 8	3,850	MAINLINE
0010	397+00	-	402+80	USH 8	700	CTH P INTERSECTION
TOTAL 0010					16,045	

LANDSCAPING

CATEGORY	STATION	TO	STATION	LOCATION	TOPSOIL SY	EROSION MAT URBAN CLASS I TYPE B SY	FERTILIZER TYPE B CWT	SEEDING MIXTURE NO. 30 LB	REMARKS
					625.0100	628.2008	629.0210	630.0130	
STAGE 1	0010	393+50	-	402+80	USH 8 LT	-	0.4	16	SHOULDERS
0010	396+86	-	397+30	USH 8 LT	25	25	0.1	1	CURB AND GUTTER
0010	397+99	-	398+46	USH 8 LT	21	21	0.1	1	CURB AND GUTTER
STAGE 2	0010	393+50	-	402+80	USH 8 RT	-	0.4	16	SHOULDERS
STAGE 3	0010	242+00	-	306+00	USH 8	-	3.0	125	SHOULDERS
0010	307+50	-	329+00	USH 8	-	-	1.0	42	SHOULDERS
0010	330+25	-	357+50	USH 8	-	-	1.1	45	SHOULDERS
0010	358+66	-	358+94	USH 8 RT	20	20	0.1	1	CURB AND GUTTER
0010	358+75	-	393+50	USH 8	-	-	1.4	58	SHOULDERS
TOTAL 0010					66	66	7.6	305	

EROSION CONTROL

CATEGORY	STATION	TO	STATION	LOCATION	628.1504 SILT FENCE LF	628.1520 SILT FENCE MAINTENANCE LF	628.7504 TEMPORARY DITCH CHECKS LF	REMARKS	
STAGE 1	0010	396+87	-	397+33	USH 8 LT	82	82	-	CTH P
0010	397+96	-	398+46	USH 8 LT	69	69	-	CTH P	
STAGE 3	0010	304+00	-	306+20	USH 8 LT	220	220	-	EXCLUSIONARY FENCING
0010	304+20	-	306+40	USH 8 RT	220	220	-	EXCLUSIONARY FENCING	
0010	307+20	-	309+40	USH 8 LT	220	220	-	EXCLUSIONARY FENCING	
0010	307+30	-	309+50	USH 8 RT	220	220	-	EXCLUSIONARY FENCING	
0010	329+04	-	330+38	USH 8 RT	20	20	-	RIVER BEND RD SOUTH (SPOT REPAIRS)	
0010	358+64	-	358+93	USH 8 RT	67	67	-	PELICAN RANCH RD SOUTH	
UNDISTRIBUTED PROJECT					112	112	50		
TOTAL 0010					1,230	1,230	50		

CONCRETE CURB & GUTTER 6-INCH SLOPED 36-INCH TYPE D

CATEGORY	STATION	TO	STATION	LOCATION	601.0557 LF	REMARKS	
STAGE 1	0010	396+87	-	397+33	USH 8 LT	82	CTH P
0010	397+96	-	398+46	USH 8 LT	69	CTH P	
STAGE 3	0010	329+04	-	330+38	USH 8 RT	20	RIVER BEND RD SOUTH (SPOT REPAIRS)
0010	358+64	-	358+93	USH 8 RT	67	PELICAN RANCH RD SOUTH	
TOTAL 0010					238		

MOBILIZATIONS EROSION CONTROL

CATEGORY	STATION	TO	STATION	LOCATION	628.1905 MOBILIZATIONS EROSION CONTROL EACH	628.1910 MOBILIZATIONS EMERGENCY EROSION CONTROL EACH
0010	242+00	-	402+80	PROJECT	4	3
TOTAL 0010					4	3

TRAFFIC CONTROL SUMMARY

LOCATION	APPROXIMATE SERVICE DAYS	643.0300 TRAFFIC CONTROL DRUMS		643.0420 TRAFFIC CONTROL BARRICADES TYPE III		643.0705 TRAFFIC CONTROL WARNING LIGHTS TYPE A		643.0715 TRAFFIC CONTROL WARNING LIGHTS TYPE C		643.0900 TRAFFIC CONTROL SIGNS		643.0920 TRAFFIC CONTROL COVERING SIGNS TYPE II		643.1000 TRAFFIC CONTROL SIGNS FIXED MESSAGE		REMARKS
		IN SERVICE	DAY	IN SERVICE	DAY	IN SERVICE	DAY	IN SERVICE	DAY	IN SERVICE	DAY	EACH	OF CYCLES	NUMBER OF SIGNS	SF	
STAGE 1A																
FLAGGING OPERATIONS FOR TEMPORARY WIDENING	3	26	78	-	-	-	-	-	-	5	15	-	-	-	36	SEE SDD "TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY"
STAGE 1B																
USH 8 LANE CLOSURE STA 396+00 TO 404+50	15	35	525	2	30	4	60	10	150	35	525	4	1	4	-	SEE SDD "TRAFFIC CONTROL, ONE LANE ROAD WITH TEMPORARY SIGNALS"
CTH P SEGMENT CLOSURE	15	-	-	12	180	6	90	-	-	6	90	-	-	-	-	SEE SDD " BARRICADES AND SIGNS FOR MAINLINE CLOSURES DETAIL C"
STAGE 2																
USH 8 LANE CLOSURE STA 396+00 TO 404+50	15	35		2	30	4	60	10	150	35	525	4	1	4	-	SEE SDD "TRAFFIC CONTROL, ONE LANE ROAD WITH TEMPORARY SIGNALS"
CTH P SEGMENT CLOSURE	15	-	-	14	210	10	150	-	-	6	90	-	-	-	-	SEE SDD " BARRICADES AND SIGNS FOR MAINLINE CLOSURES DETAIL C"
STAGE 3																
DETOUR	115	-	-	4	460	8	920	-	-	64	7360	3	1	3	36	SEE SDD " BARRICADES AND SIGNS FOR MAINLINE CLOSURES DETAIL B"
RIVER BEND RD ACCESS CLOSURE	115	-	-	10	1150	12	1380	-	-	6	690	-	-	-	-	SEE SDD " BARRICADES AND SIGNS FOR SIDEROAD CLOSURES DETAIL 1"
PELICAN RANCH RD ACCESS CLOSURE	115	-	-	4	460	8	920	-	-	6	690	-	-	-	-	SEE SDD " BARRICADES AND SIGNS FOR SIDEROAD CLOSURES DETAIL 4"
TOTAL 0010				603	2,520		3,580		300		9,985		11		72	

PAVEMENT MARKING

CATEGORY	STATION	TO	STATION	LOCATION	646.2040 MARKING LINE GROOVED WET REF EPOXY 6-INCH		646.4020 MARKING LINE EPOXY 10-INCH		646.5020 MARKING ARROW EPOXY		646.5120 MARKING WORD EPOXY		646.6120 MARKING STOP LINE EPOXY 18-INCH		646.6466 COLD WEATHER MARKING EPOXY 6-INCH		646.6470 COLD WEATHER MARKING EPOXY 10-INCH		REMARKS
					LF	LF	LF	LF	LF	LF	LF	LF	LF	LF					
STAGE 3																			
0010	242+00	-	306+04	USH 8	6,843	-	-	-	-	-	-	-	-	-	-	-	-	-	YELLOW CENTERLINE
0010	242+00	-	306+04	USH 8	12808	-	-	-	-	-	-	-	-	-	-	-	-	-	WHITE EDGELINE
0010	307+59	-	329+00	USH 8	1,086	-	-	-	-	-	-	-	-	-	-	-	-	-	YELLOW CENTERLINE
0010	307+59	-	329+00	USH 8	4,275	-	-	-	-	-	-	-	-	-	-	-	-	-	WHITE EDGELINE
0010	329+00	-	357+50	USH 8	4,632	-	-	-	-	-	-	-	-	-	-	-	-	-	YELLOW CENTERLINE
0010	330+25	-	357+50	USH 8	5,412	-	-	-	-	-	-	-	-	-	-	-	-	-	WHITE EDGELINE
0010	357+50	-	406+00	USH 8	2,813	-	-	-	-	-	-	-	-	-	-	-	-	-	YELLOW CENTERLINE
0010	357+50	-	402+80	USH 8	4,430		115		1		1								WB LANE
0010	357+50	-	402+80	USH 8	4,602														EB LANE
0010	397+00	-	400+00	USH 8	60		42						37						CTH P INTERSECTION
0010	UNDISTRIBUTED			USH 8	-										4,697			16	
TOTAL 0010					46,961		157		1		1		37		4,697			16	

3

3

CONSTRUCTION STAKING

CATEGORY	STATION	TO	STATION	LOCATION	650.4500	650.5000	650.5500	650.8000	650.9911.01
					CONSTRUCTION STAKING SUBGRADE LF	CONSTRUCTION STAKING BASE LF	CONSTRUCTION STAKING CURB GUTTER AND CURB & GUTTER LF	CONSTRUCTION STAKING RESURFACING REFERENCE LF	CONSTRUCTION STAKING SUPPLEMENTAL CONTROL (PROJECT) (01. 1590-12-76) EACH
STAGE 1									
0010	393+50	-	402+80	USH 8	-	930	-	930	-
STAGE 3									
0010	242+00	-	306+40	USH 8	6440	6440	-	6440	-
0010	307+31	-	327+00	USH 8	1969	1969	-	1969	-
0010	327+00	-	393+50	USH 8	-	6650	-	6650	-
0010	-	-	-	PROJECT LIMITS	-	-	238	-	1
TOTAL 0010					8,409	15,989	238	15,989	1

MARKING REMOVAL LINE 4-INCH

CATEGORY	STATION	TO	STATION	LOCATION	646.9000	REMARKS
					LF	
0010	394+50	-	396+00	USH 8	188	CENTERLINE
0010	404+50	-	406+00	USH 8	38	CENTERLINE
TOTAL 0010					225	

TEMPORARY MARKING

CATEGORY	STATION	TO	STATION	LOCATION	643.3165	643.3850	REMARKS
					TEMPORARY MARKING LINE PAINT 6-INCH LF	TEMPORARY MARKING STOP LINE REMOVABLE TAPE 18-INCH LF	
STAGE 1							
0010	396+00	-	404+50	USH 8	850	-	YELLOW CENTERLINE
0010	396+00	-	404+50	USH 8	850	-	WHITE EDGELINE
0011		394+50		USH 8	-	16	STOP LINE
0010		406+00		USH 8	-	16	STOP LINE
STAGE 2							
0010	396+00	-	404+50	USH 8	850	-	YELLOW CENTERLINE
0010	396+00	-	404+50	USH 8	850	-	WHITE EDGELINE
TOTAL 0010					3,400	32	

SAWING

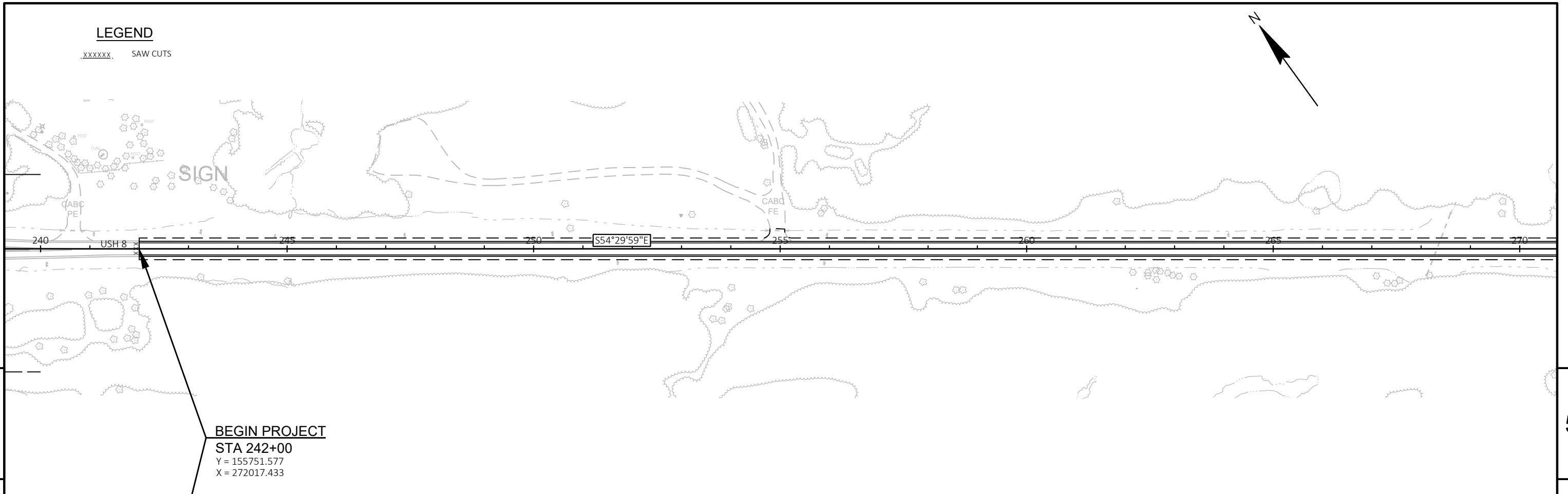
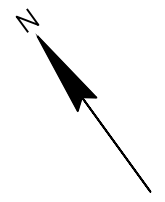
CATEGORY	STATION	TO	STATION	OFFSET	LOCATION	690.0150	690.0250	REMARKS	
						SAWING ASPHALT LF	SAWING CONCRETE LF		
STAGE 1									
0010	396+00			LT/RT	USH 8	43	-	BEGINNING STAGE 1	
0010	395+68	-	403+32	LT	USH 8	763	-	CTH P	
0010	396+87	-	397+33	LT	USH 8	89	-	CURB AND GUTTER REPLACEMENT	
0010	397+96	-	398+46	LT	USH 8	76	-	CURB AND GUTTER REPLACEMENT	
0010	402+80			LT/RT	USH 8	32	-	END OF PROJECT	
STAGE 3									
0010	242+00			LT/RT	USH 8	32	-	BEGINNING OF PROJECT	
0010	327+00	-	331+54	RT	USH 8	454	-	RIVER BEND ROAD	
0010	328+28	-	332+12	LT	USH 8	384	-	RIVER BEND ROAD	
0010	356+00	-	360+00	RT	USH 8	401	-	PELICAN RANCH ROAD	
0010	356+63	-	360+00	LT	USH 8	337	-	PELICAN RANCH ROAD	
0010	358+64	-	358+93	RT	USH 8	75	-	CURB AND GUTTER REPLACEMENT	
UNDISTRIBUTED						PROJECT	-	12	SPOT CURB AND GUTTER REPAIRS
TOTAL 0010						2,686	12		

TEMPORARY TRAFFIC SIGNALS FOR BRIDGES (STRUCTURE) (01. STATION 398+00)

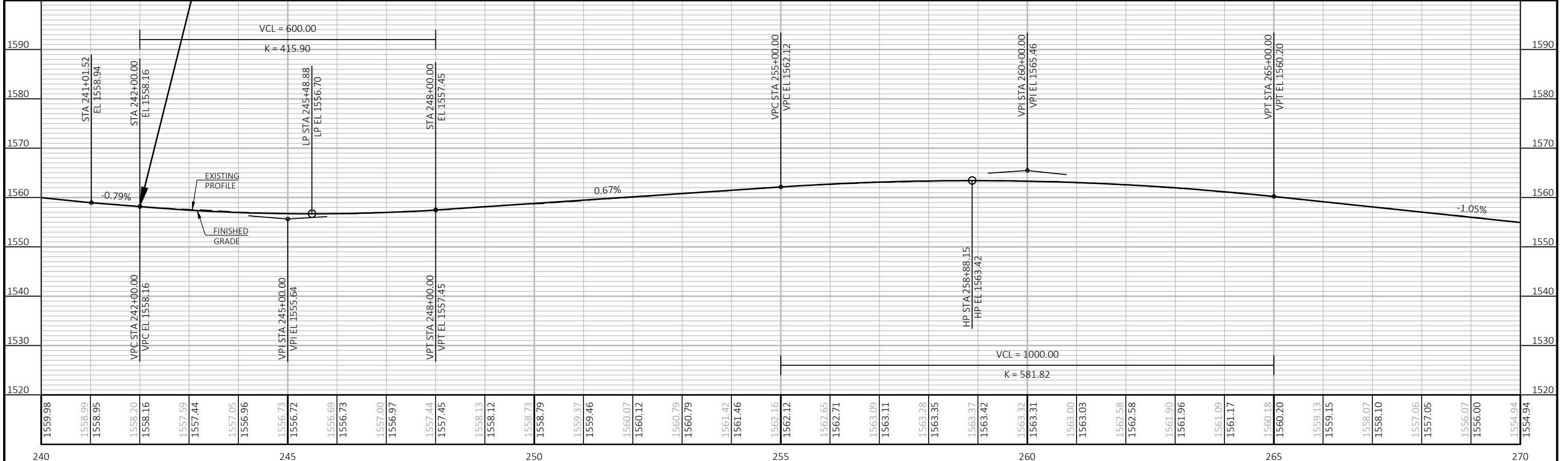
CATEGORY	STATION	TO	STATION	LOCATION	661.0101.01	REMARKS
					EACH	
0010	396+00	-	405+00	USH 8	1	CTH P INTERSECTION
TOTAL 0010					1	

LEGEND

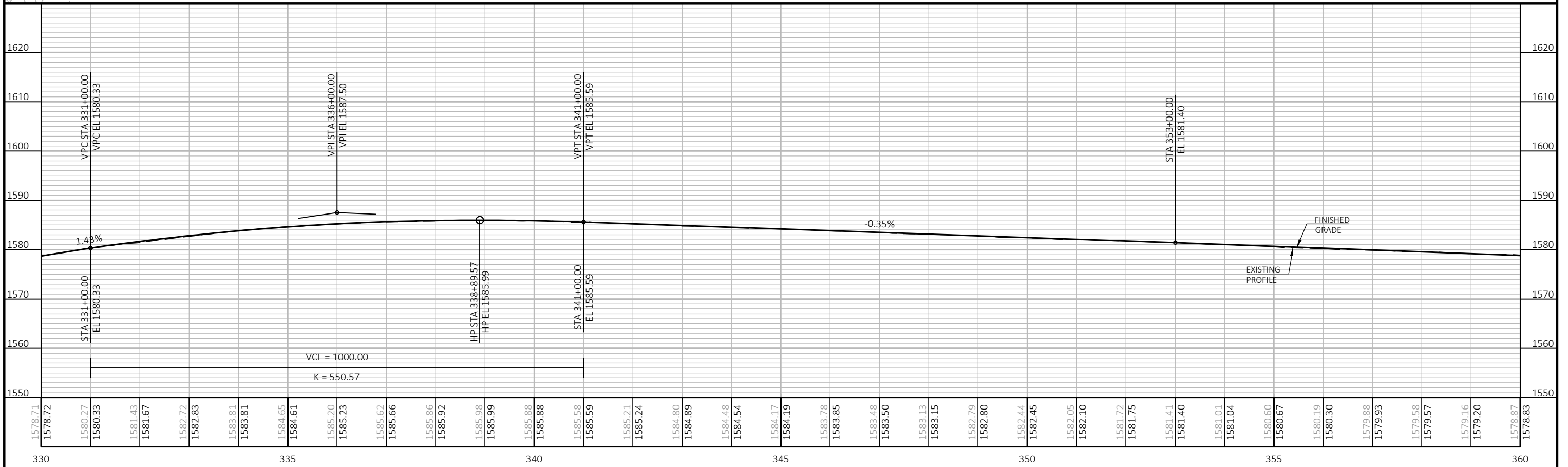
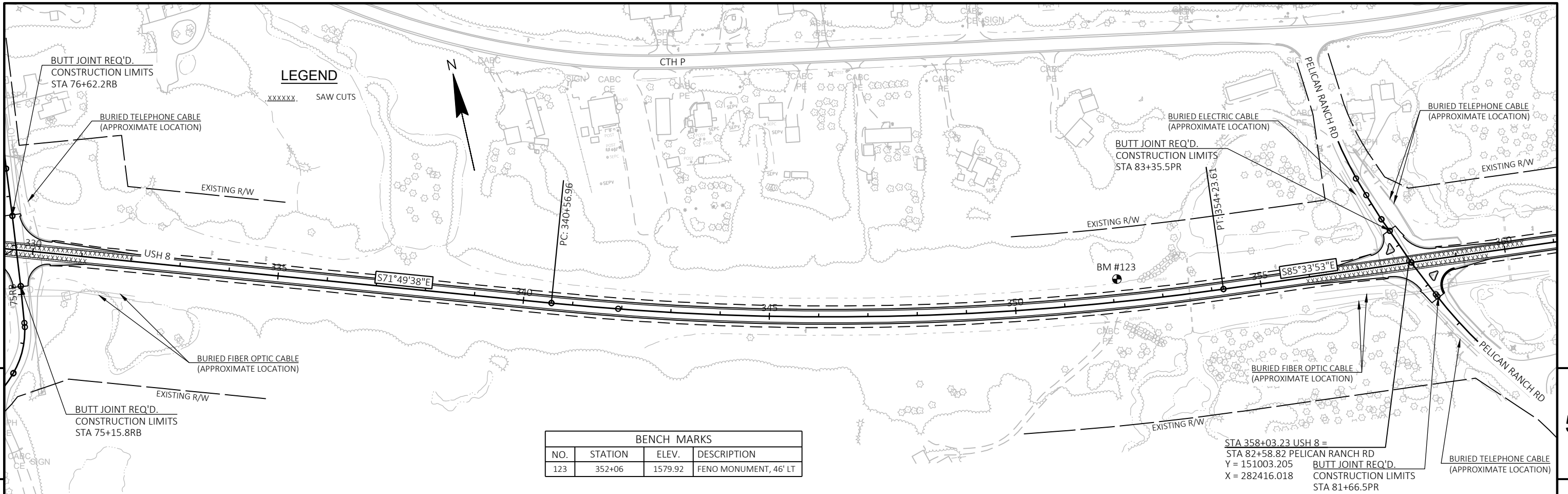
xxxxxx SAW CUTS



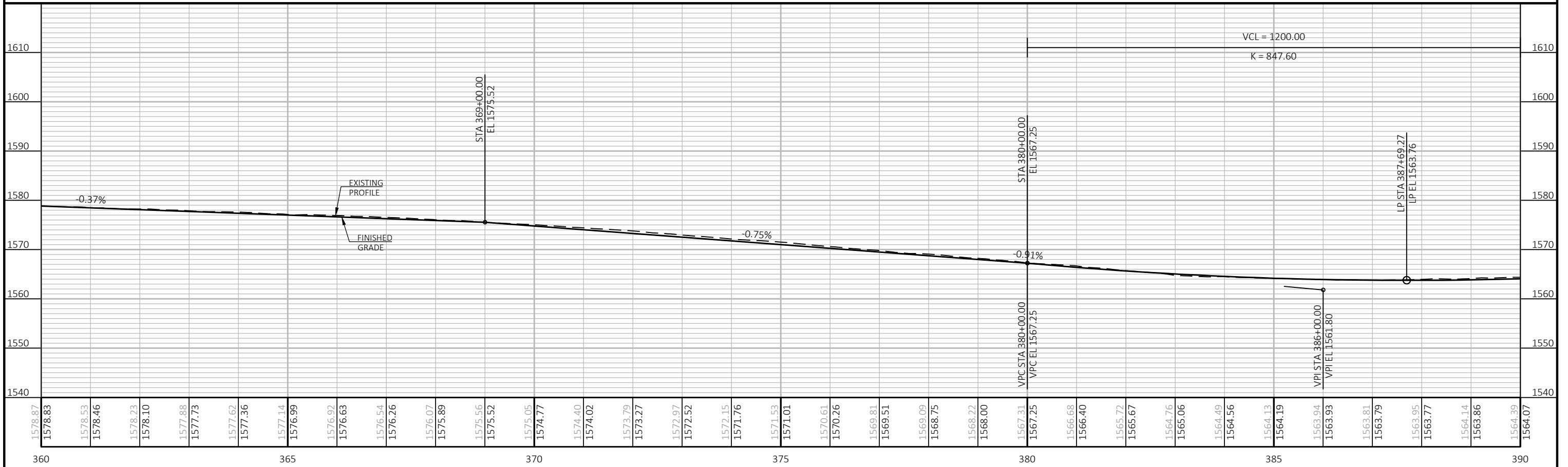
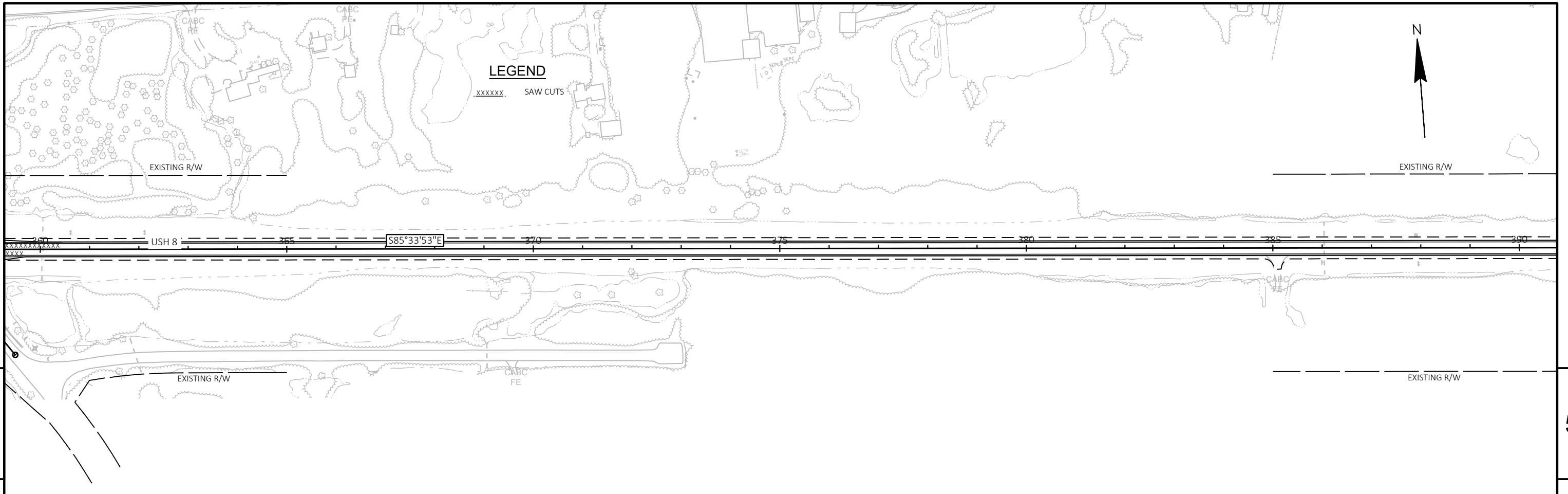
BEGIN PROJECT
STA 242+00
 Y = 155751.577
 X = 272017.433



PROJECT NO: 1590-12-76	HWY: USH 8	COUNTY: ONEIDA	PLAN AND PROFILE: USH 8	SHEET	E
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PROJECT NO: 1590-12-76	HWY: USH 8	COUNTY: ONEIDA	PLAN AND PROFILE: USH 8	SHEET E
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PROJECT NO: 1590-12-76	HWY: USH 8	COUNTY: ONEIDA	PLAN AND PROFILE: USH 8	SHEET	E
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LEGEND

xxxxxx SAW CUTS

BURIED TELEPHONE CABLE
(APPROXIMATE LOCATION)

BUTT JOINT REQ'D.
CONSTRUCTION LIMITS
STA 91+03.2CP

BURIED FIBER OPTIC CABLE
(APPROXIMATE LOCATION)

EXISTING R/W

BURIED FIBER OPTIC CABLE
(APPROXIMATE LOCATION)

EXISTING R/W

EXISTING R/W

STA 397+95.84 USH 8 =
STA 90+01.88 CTH P
Y = 150694.443
X = 286396.668

END PROJECT
STA 402+80
Y = 150657.002
X = 286879.379

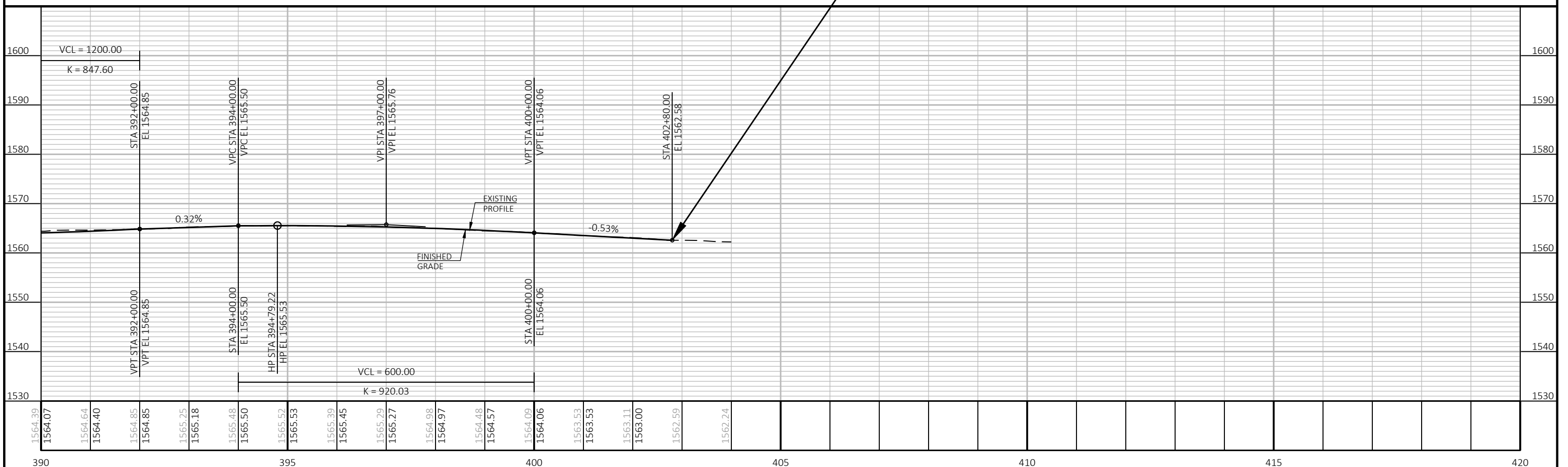
BENCH MARKS			
NO.	STATION	ELEV.	DESCRIPTION
129	401+45	1566.94	FENO MONUMENT, 84' LT

N



5

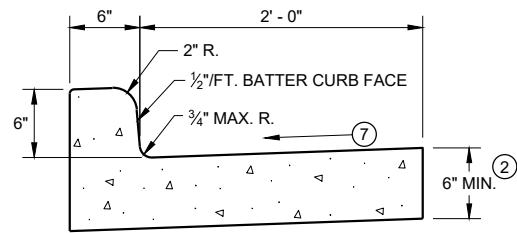
5



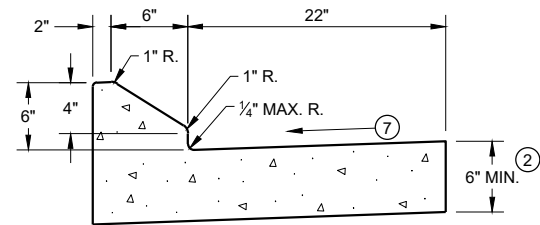
PROJECT NO: 1590-12-76	HWY: USH 8	COUNTY: ONEIDA	PLAN AND PROFILE: USH 8	SHEET	E
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Standard Detail Drawing List

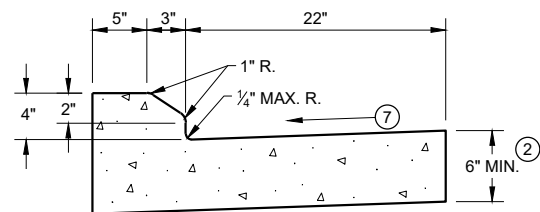
08D01-23A	CONCRETE CURB & GUTTER
08D01-23B	CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS
08D21-01	DRIVEWAYS WITHOUT CURB & GUTTER
08E08-03	TYPICAL INSTALLATIONS OF EROSION BALES / TEMPORARY DITCH CHECKS
08E09-06	SILT FENCE
09A01-14A	AT-GRADE SIDE ROAD INTERSECTION, TYPES "B1", "B2", "C" AND D AND TEE INTERSECTION BYPASS LANE
09A01-14B	AT-GRADE SIDE ROAD INTERSECTION, TYPE "A1" & "A2"
09G02-05B	BRIDGE TEMPORARY TRAFFIC SIGNAL INSTALLATION
09G02-05C	BRIDGE TEMPORARY TRAFFIC SIGNAL INSTALLATION
13A11-04A	CENTERLINE RUMBLE STRIPS - ASPHALT
13A11-04D	CENTERLINE RUMBLE STRIPS - INTERSECTIONS, DRIVEWAYS, BRIDGES, RAILROADS
13B02-09A	CONCRETE PAVEMENT APPROACH SLAB
13C19-03	HMA LONGITUDINAL JOINTS
15C02-09A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-09B	BARRICADES AND SIGNS FOR VARIOUS CLOSURES
15C02-09C	DETOUR SIGNING FOR MAINLINE CLOSURES
15C02-09F	ADVANCED WIDTH RESTRICTION SIGNING
15C03-05	BARRICADES AND SIGNS FOR SIDEROAD CLOSURES
15C04-05	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 M.P.H. OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC
15C07-15B	PAVEMENT MARKING WORDS
15C07-15C	PAVEMENT MARKING ARROWS
15C08-23A	PERMANENT LONGITUDINAL PAVEMENT MARKINGS
15C08-23B	TEMPORARY LONGITUDINAL PAVEMENT MARKING
15C08-23D	PAVEMENT MARKING (TURN LANES)
15C11-10B	CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS
15C33-04	STOP LINE AND CROSSWALK PAVEMENT MARKING
15C35-06A	PAVEMENT MARKING (INTERSECTIONS)
15D28-04	TRAFFIC CONTROL, WORK ON SHOULDER OR PARKING LANE, UNDIVIDED ROADWAY
15D33-09	TRAFFIC CONTROL, ONE LANE ROAD WITH TEMPORARY SIGNALS
15D39-02	TRAFFIC CONTROL, DROP-OFF SIGNING
15D44-02	TRAFFIC CONTROL, SIGNING ON ROADWAYS WITH MILLED SURFACES
15D45-03	TRAFFIC CONTROL, SIGNING ON ROADWAYS WITH LOOSE GRAVEL
15D51-01	TRAFFIC CONTROL, MOBILE OPERATIONS ON AN UNDIVIDED ROADWAY



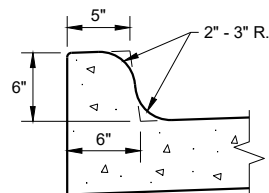
TYPES A¹ & D



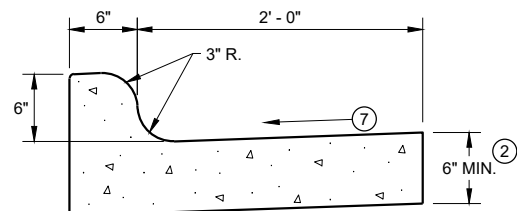
6" SLOPED CURB TYPES G¹ & J



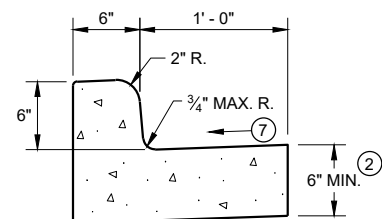
4" SLOPED CURB TYPES G¹ & J



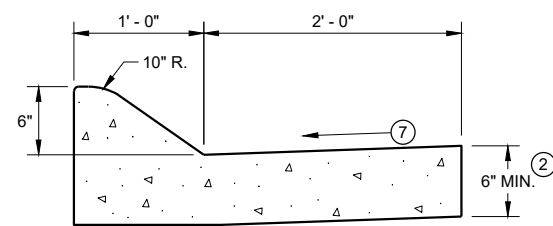
TYPES K¹ & L
(OPTIONAL CURB SHAPE)



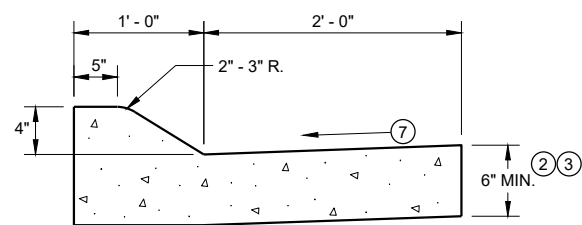
TYPES K¹ & L
CONCRETE CURB AND GUTTER 30"



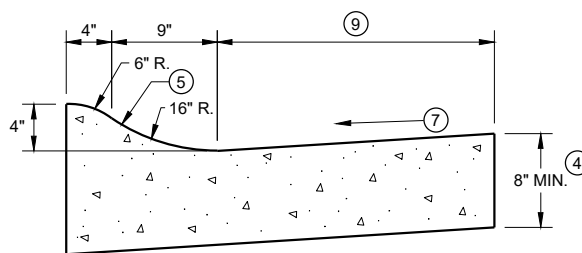
TYPES A¹ & D
CONCRETE CURB AND GUTTER 18"



6" SLOPED CURB TYPES A¹ & D

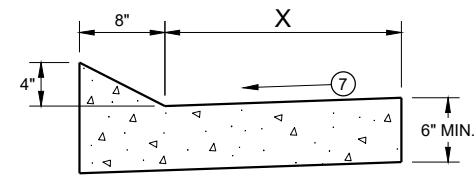


4" SLOPED CURB TYPES A¹ & D
CONCRETE CURB AND GUTTER 36"



4" SLOPED CURB TYPES R¹ & T

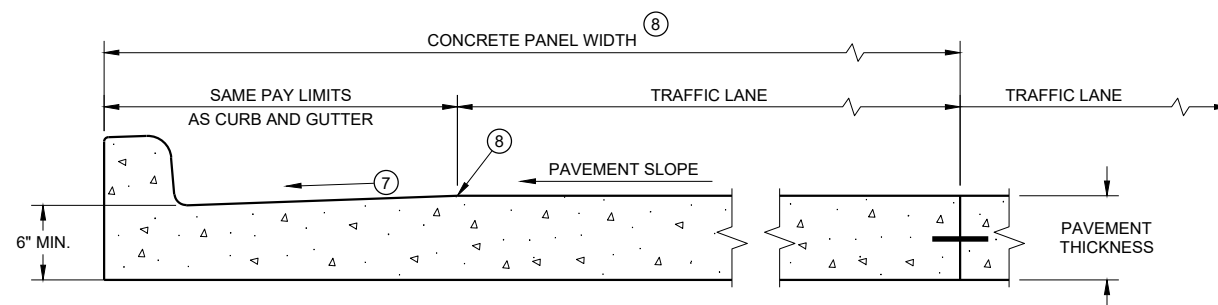
TBT & TBTT	X
30"	22"
36"	28"



TYPES TBT & TBTT¹
CONCRETE CURB AND GUTTER

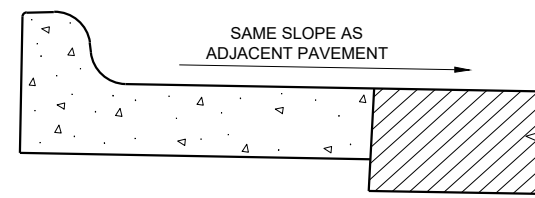
PAVEMENT THICKNESS AND MAXIMUM CONCRETE PANEL WIDTH TABLE

PAVEMENT THICKNESS	MAXIMUM PANEL WIDTH
LESS THAN 10"	12'
10" & ABOVE	15'



PARTIAL SECTION OF PAVEMENT* WITH INTEGRAL CURB AND GUTTER

* BIKE LANE IS NOT SHOWN



REVERSE SLOPE GUTTER⁶
(TYPICAL FOR ALL CURB & GUTTER TYPES)

GENERAL NOTES

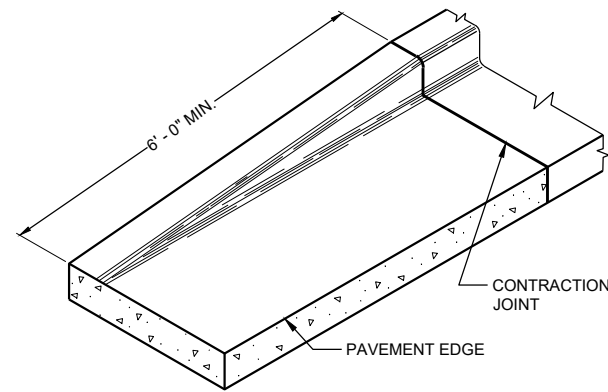
DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

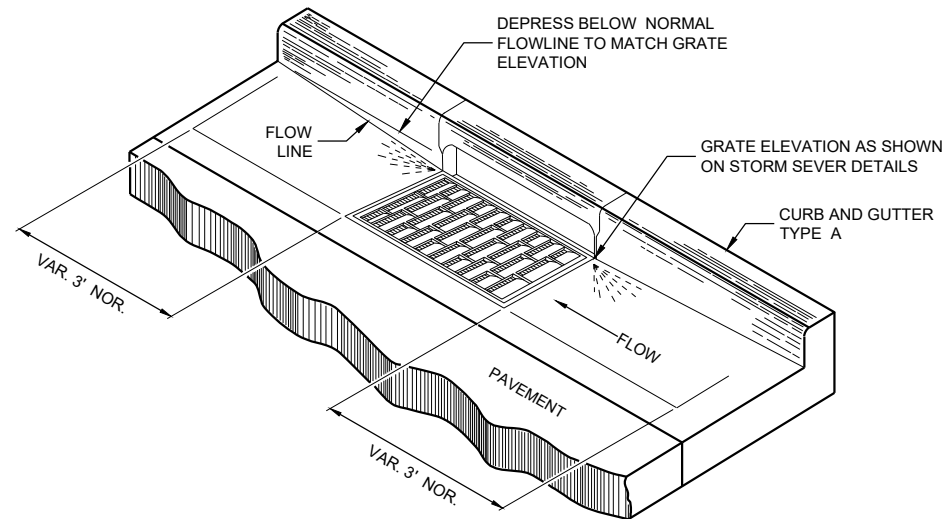
INTEGRAL CURB AND GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB AND GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ③ USE 8" MINIMUM GUTTER THICKNESS WHEN USED WITH AN ADJACENT CONCRETE TRUCK APRON PLACED BEHIND BACK OF CURB.
- ④ THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑤ UNLESS OTHERWISE NOTED, FOR STAKING PURPOSES THE FACE OF CURB IS 6" FROM THE BACK OF CURB.
- ⑥ WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.
- ⑦ USE 4% GUTTER CROSS SLOPE UNLESS OTHERWISE NOTED IN THE PLANS.
- ⑧ INCLUDE LONGITUDINAL JOINT AND TIE BARS ALONG LANE EDGE WHEN CONCRETE PANEL WIDTH EXCEEDS THE MAXIMUM WIDTH PER TABLE BELOW. LONGITUDINAL JOINT(S) ARE NOT ALLOWED WITHIN TRAFFIC LANES AND BIKE LANES. LONGITUDINAL JOINT MAY BE SAWED.
- ⑨ CONCRETE CURB AND GUTTER 4-INCH SLOPED 30-INCH TYPE "R" AND "T" = 17 INCHES
CONCRETE CURB AND GUTTER 4-INCH SLOPED 36-INCH TYPE "R" AND "T" = 23 INCHES



END SECTION CURB AND GUTTER



DETAIL OF CURB AND GUTTER AT INLETS

(TYPICAL H INLET COVER SHOWN)

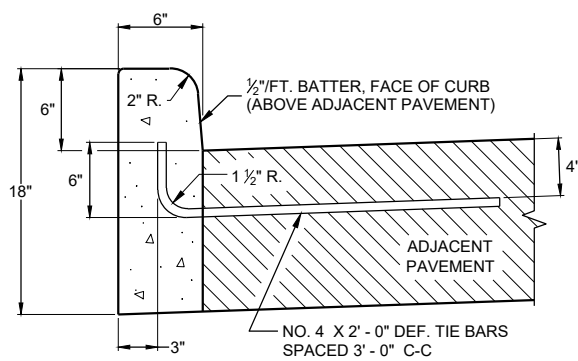
GENERAL NOTES

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

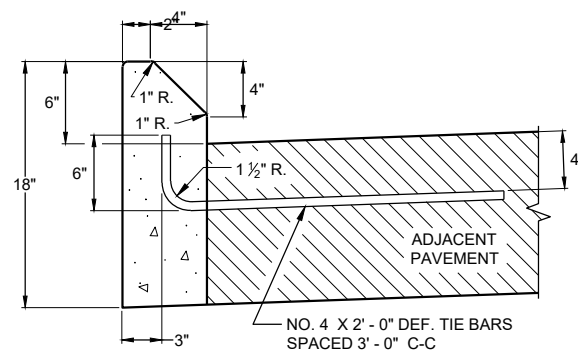
PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

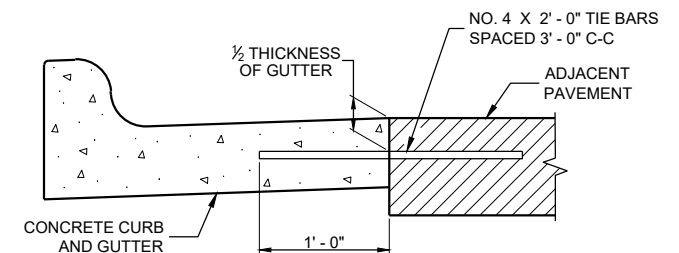
- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑩ REFER TO SDD 08D18 AND 08D19 FOR ADDITIONAL DRIVEWAY ENTRANCE CURB DETAILS.
- ⑪ PLACE 1" THICK EXPANSION JOINT MATERIAL BETWEEN VERTICAL FACE CURB TYPES EXTENDING FROM THE TOP OF CURB TO 1 INCH BELOW THE ADJOINING CONCRETE SURFACE. RIGID CONCRETE STRUCTURES INCLUDE RAISED CONCRETE MEDIANS, CONCRETE SAFETY ISLANDS, SPLITTER ISLANDS, OR LOCATIONS IDENTIFIED ON THE PLANS.



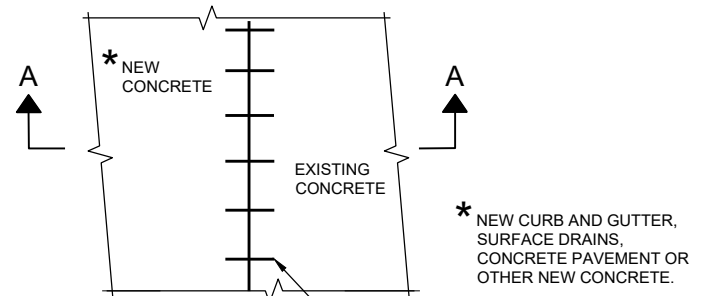
TYPES A^① & D



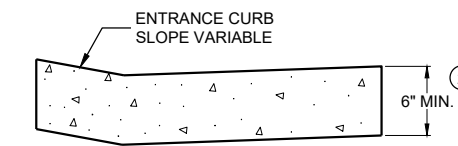
**TYPES G^① & J
CONCRETE CURB**



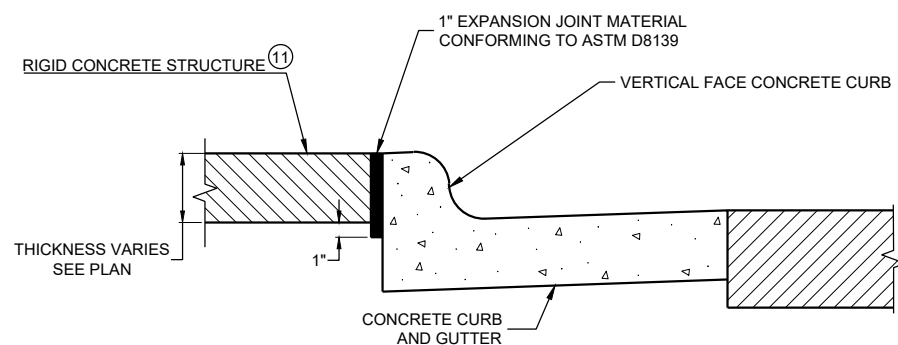
TYPICAL TIE BAR LOCATION^①



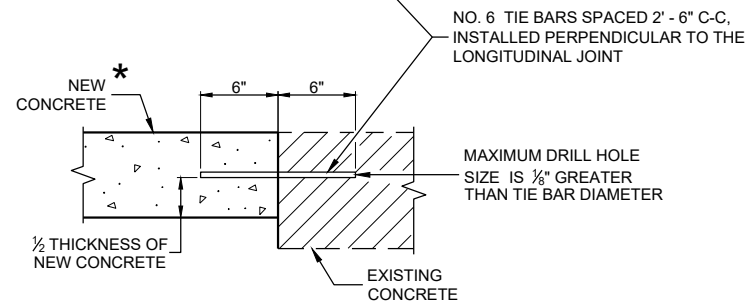
PLAN VIEW



**DRIVEWAY ENTRANCE CURB^⑩
(WHEN DIRECTED BY THE ENGINEER)**



EXPANSION JOINT DETAIL FOR VERTICAL CURB ABUTTING A RIGID STRUCTURE^⑪



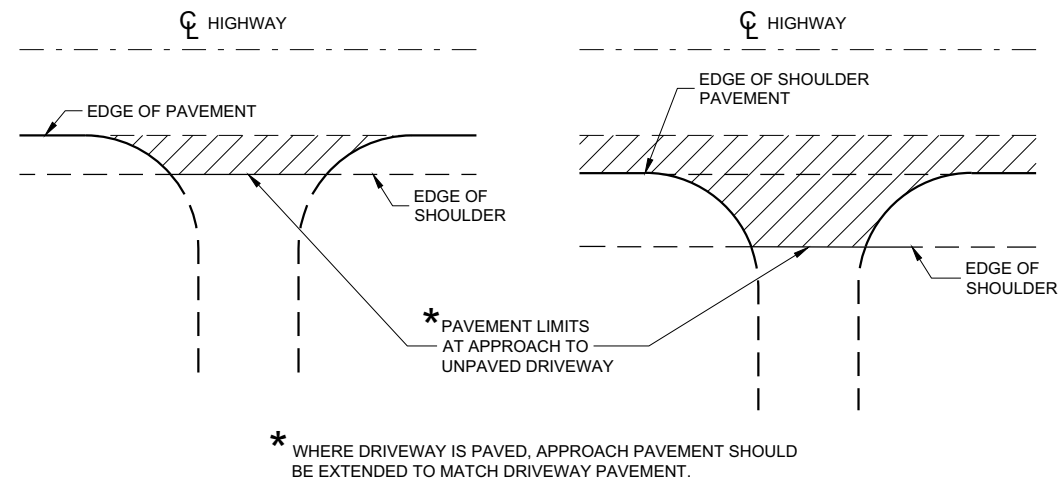
**SECTION A - A
TIE BARS DRILLED INTO EXISTING PAVEMENT**

CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE May 2023 /S/ Rodney Taylor
ROADWAY STANDARDS DEVELOPMENT ENGINEER

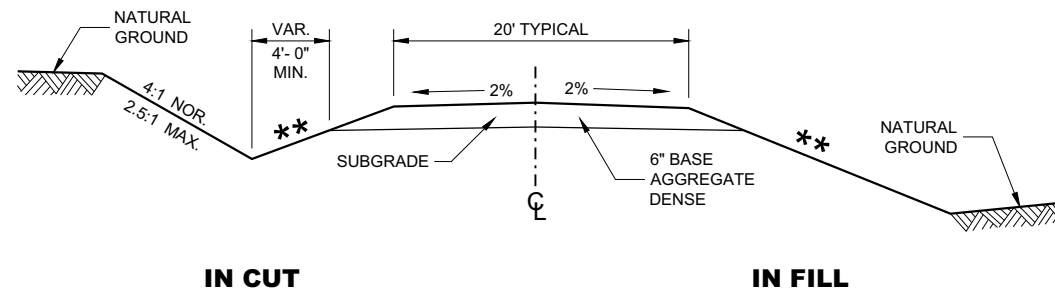
FHWA



PLAN VIEW
(UNPAVED SHOULDER ON HIGHWAY)

PLAN VIEW
(PAVED SHOULDER ON HIGHWAY)

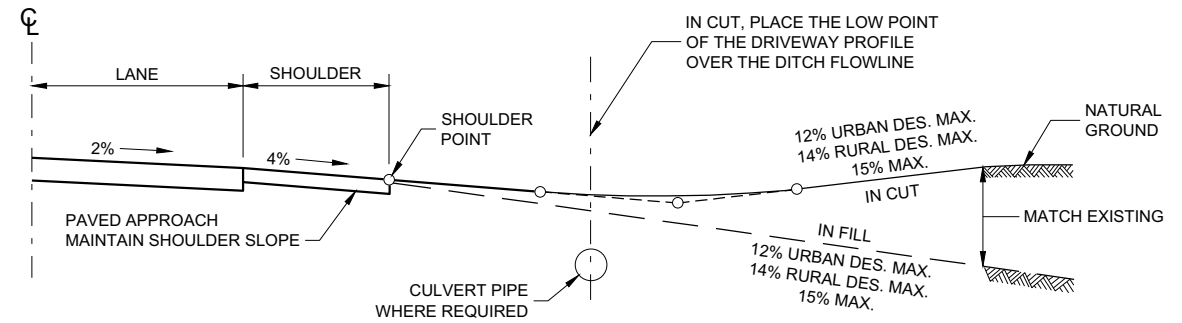
**RURAL DRIVEWAY INTERSECTION DETAIL
(NO CURB AND GUTTER OR SIDEWALK)**



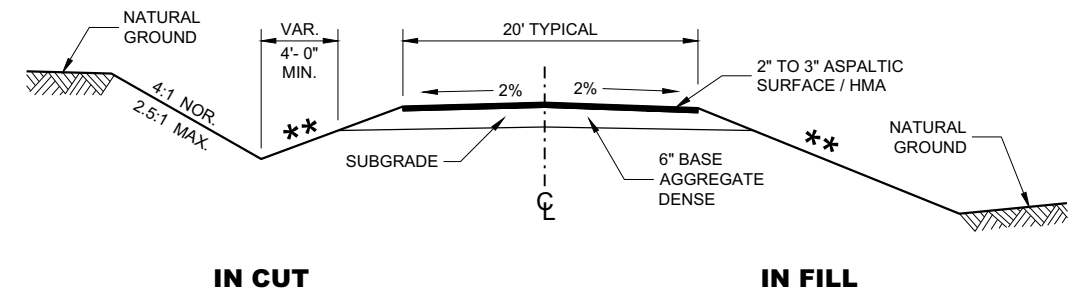
**TYPICAL CROSS SECTION FOR
PRIVATE DRIVE OR FIELD ENTRANCE
AGGREGATE SURFACE**

** SLOPE CAN VARY WITH SPEED. SEE 11-45-30.6.2

POSTED SPEED MPH	MAX. SLOPE
<35	4:1
≥ 35 TO < 60	6:1
≥60	10:1



TYPICAL DRIVEWAY PROFILES



**TYPICAL CROSS SECTION FOR
PRIVATE DRIVE OR FIELD ENTRANCE
ASPHALTIC SURFACE**

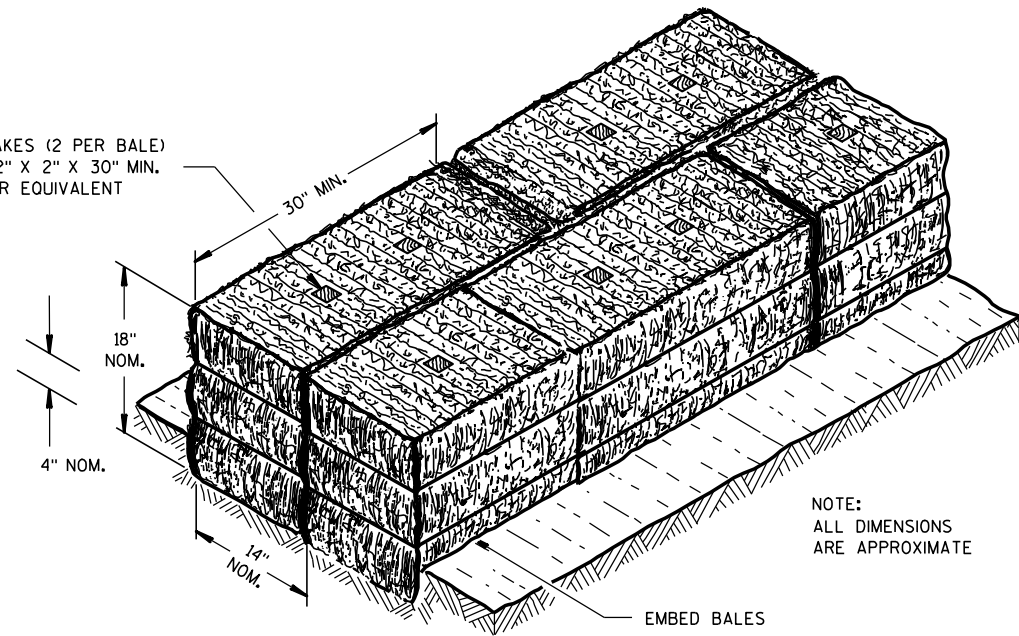
DRIVEWAYS WITHOUT CURB AND GUTTER

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
December 2017 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR

FHWA

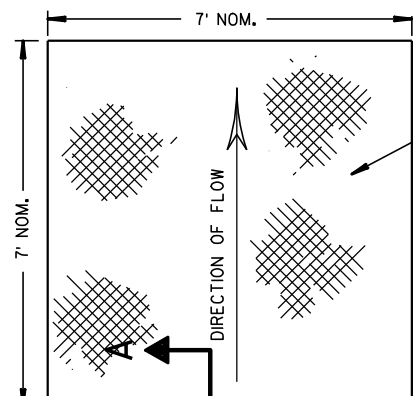
WOOD STAKES (2 PER BALE)
NOMINAL 2" X 2" X 30" MIN.
LENGTH OR EQUIVALENT



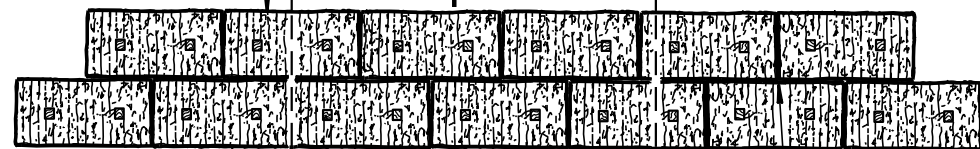
NOTE:
ALL DIMENSIONS
ARE APPROXIMATE

EMBED BALES

SECTION A-A



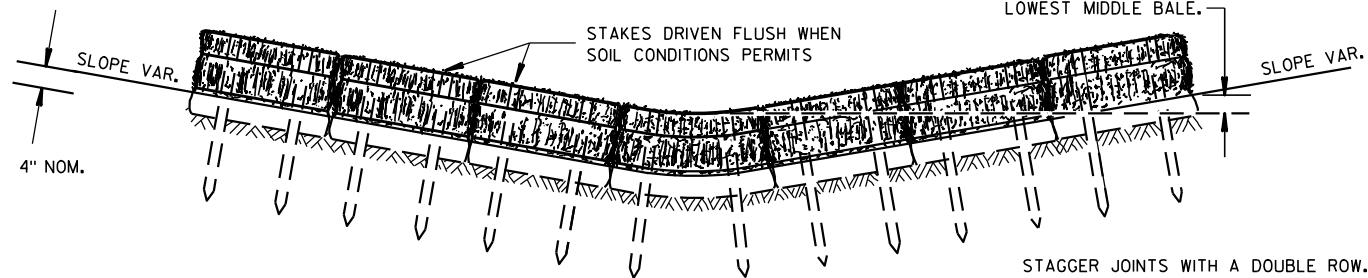
FOR SCOUR PROTECTION USE:
EROSION MAT FOR CHANNEL LINING.
LAP MAT UNDER UPSTREAM BALES
AND SECURE FABRIC WITH WOOD STAKES,
AT 3-FOOT INTERVALS.



STAGGER JOINTS BETWEEN ADJACENT
ROWS OF BALES.

PLAN VIEW

BOTTOM ELEVATION OF END BALE SHALL
BE EQUAL TO OR GREATER THAN TOP OF
LOWEST MIDDLE BALE.



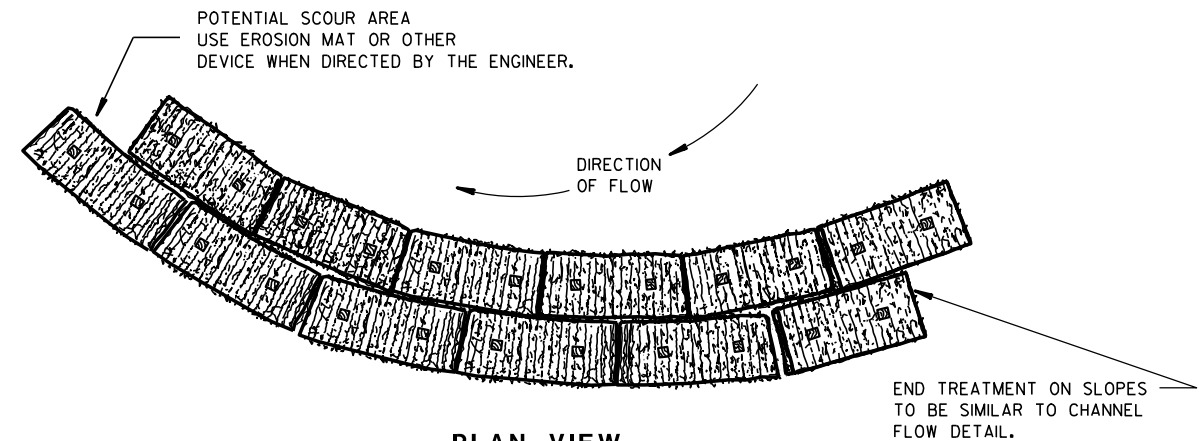
FRONT ELEVATION

TEMPORARY DITCH CHECK USING EROSION BALES ①

GENERAL NOTES

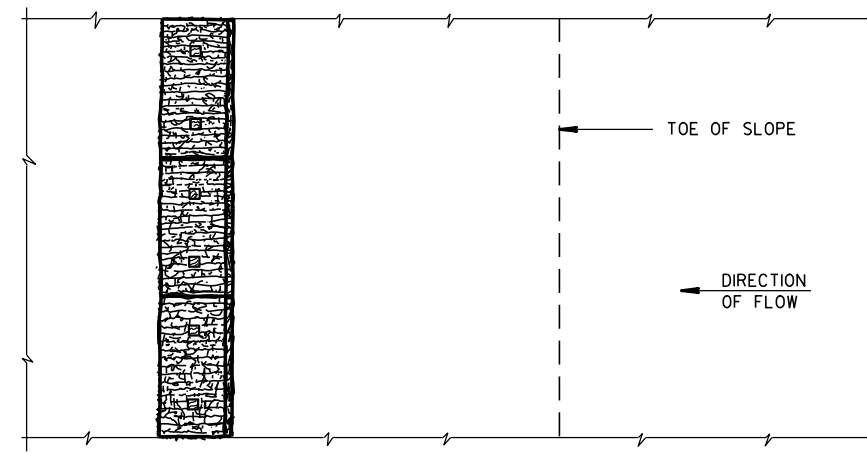
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

- ① TEMPORARY DITCH CHECKS EITHER EROSION BALES OR MANUFACTURED SHALL BE PAID FOR UNDER THE BID ITEM OF TEMPORARY DITCH CHECK. THE DEPARTMENT WILL NOT PAY FOR TEMPORARY DITCH CHECKS CONSTRUCTED OF A SINGLE ROW OF EROSION BALES.

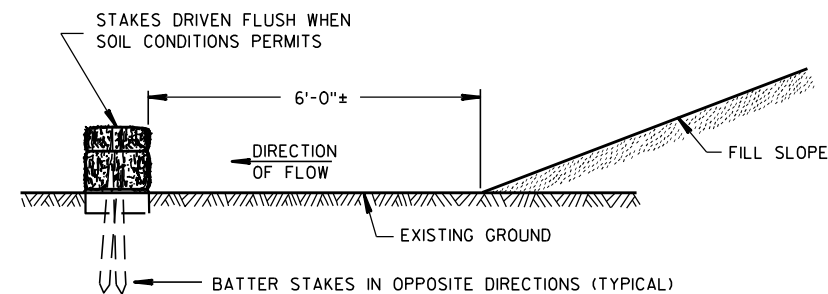


PLAN VIEW

WHEN ALTERING THE DIRECTION OF FLOW



PLAN VIEW



FRONT ELEVATION

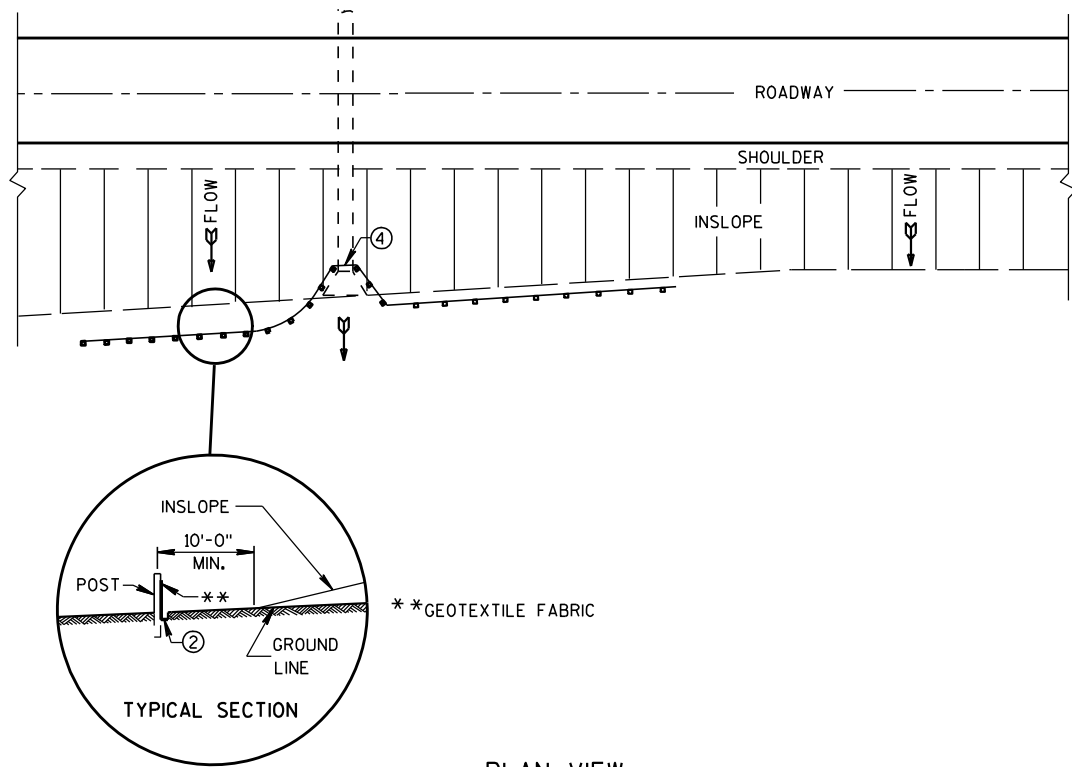
WHEN EXISTING GROUND SLOPES AWAY FROM FILL SLOPE

EROSION BALES FOR SHEET FLOW

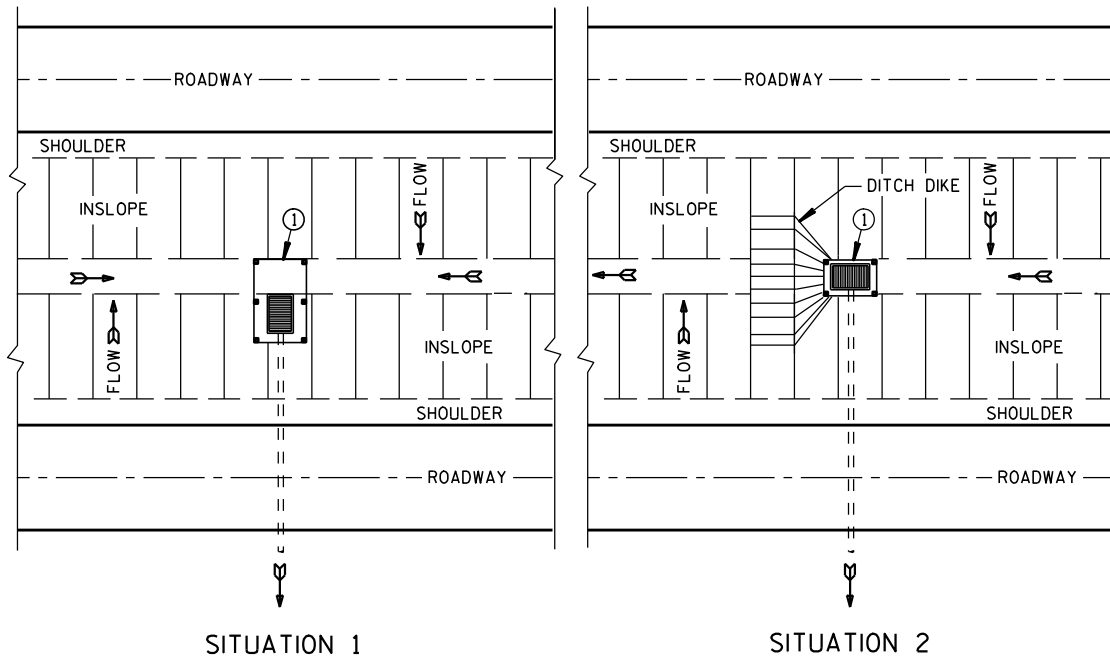
TYPICAL INSTALLATIONS OF
EROSION BALES / TEMPORARY
DITCH CHECKS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
 6/04/02 /S/ Beth Canestra
 DATE CHIEF ROADWAY DEVELOPMENT ENGINEER
 FHWA



PLAN VIEW
TYPICAL APPLICATION OF SILT FENCE

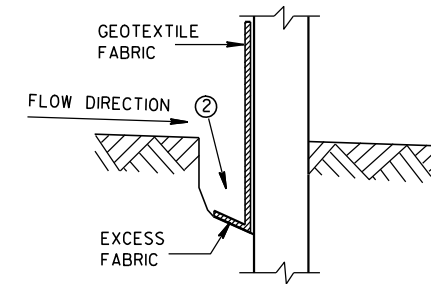


SITUATION 1 SITUATION 2
PLAN VIEW
SILT FENCE AT MEDIAN SURFACE DRAINS

GENERAL NOTES

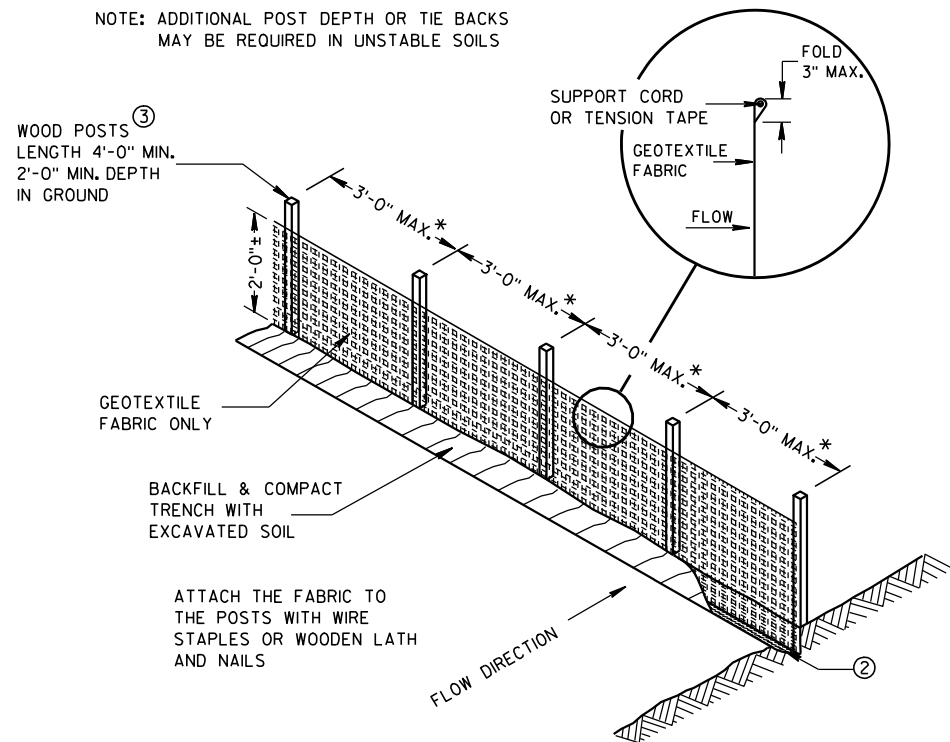
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

- ① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- ③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1 1/8" X 1 1/8" OF OAK OR HICKORY.
- ④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- ⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.

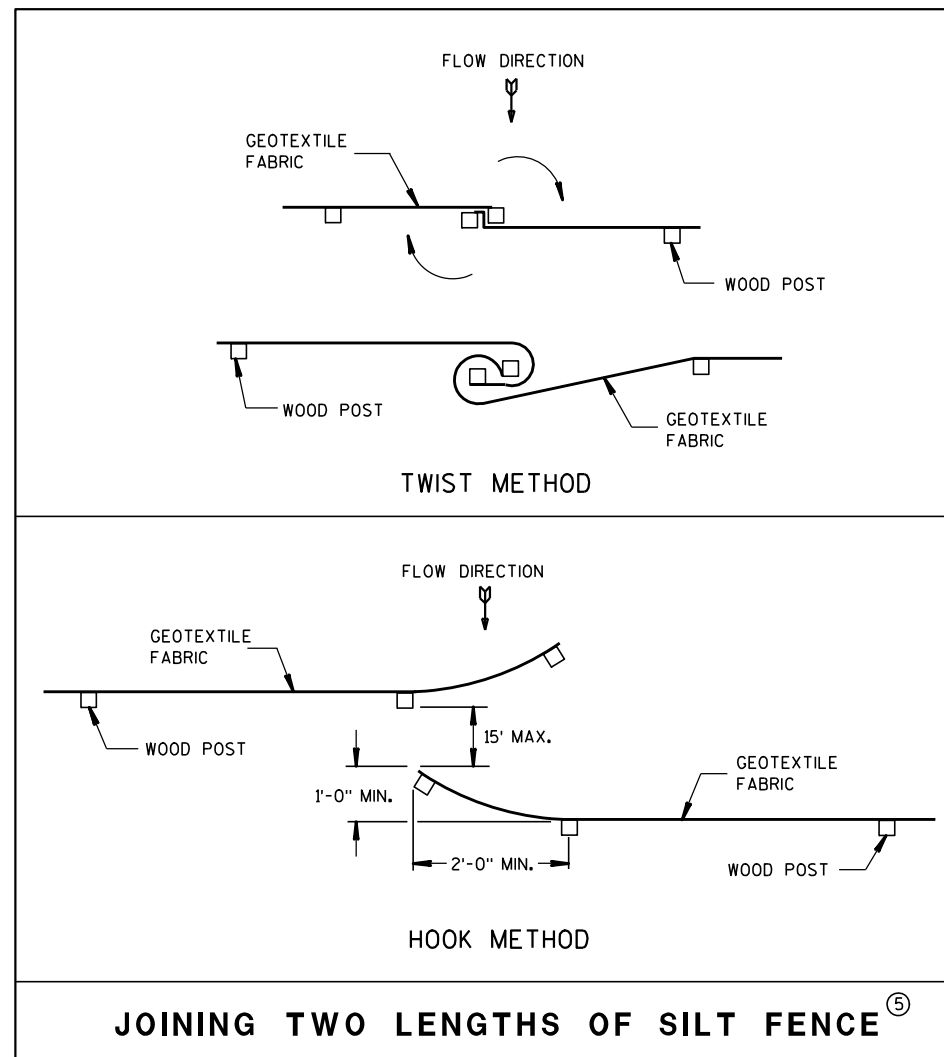


TRENCH DETAIL

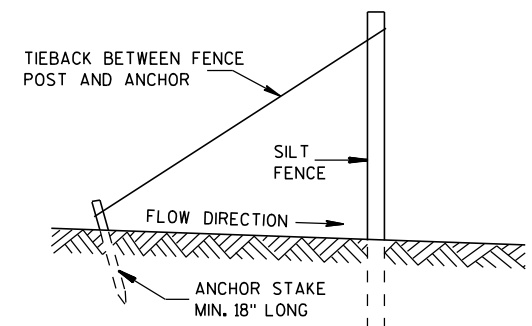
NOTE: ADDITIONAL POST DEPTH OR TIE BACKS MAY BE REQUIRED IN UNSTABLE SOILS



SILT FENCE



JOINING TWO LENGTHS OF SILT FENCE ⑤

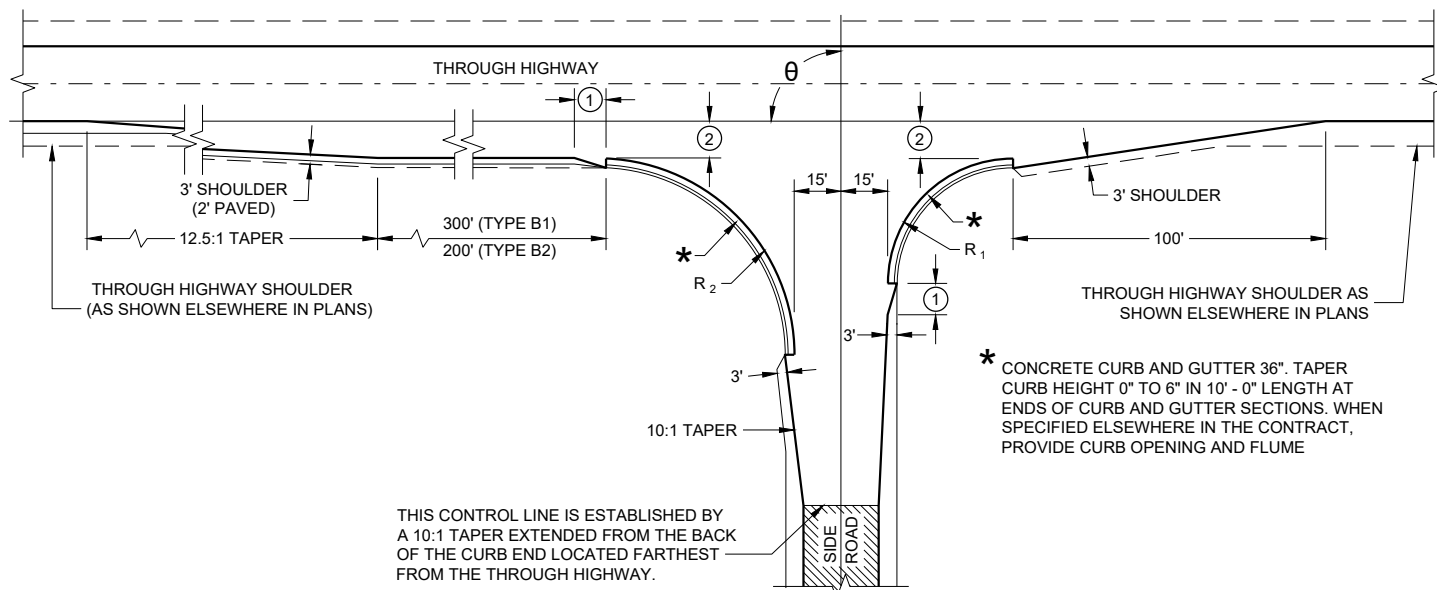


SILT FENCE TIE BACK
(WHEN REQUIRED BY THE ENGINEER)

SILT FENCE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
4-29-05 /S/ Beth Canestra
DATE CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA



TYPE "B1" AND "B2"

RADI DIMENSIONS FOR TYPES "B1", "B2", "C" AND "D" INTERSECTIONS

θ	R_1	R_2
65 - 70	35	70
71 - 80	40	70
81 - 90	40	60
91 - 100	50	55
101 - 110	60	45

GENERAL NOTES

DESIGNS MAY BE USED INTERCHANGEABLY IN COMBINATION OR SEPARATELY FOR ANY ONE COMPLETE INTERSECTION DEPENDING UPON INTERSECTION ANGLE AND SURFACING OF EACH APPROACH ROADWAY.

SIDE ROAD SURFACING NOTE

WHEN THE SIDE ROAD IS NOT PRESENTLY PAVED, PAVEMENT SHALL BE PLACED TO THE LIMITS SHOWN UNLESS OTHERWISE PROVIDED IN THE CONTRACT. WHERE THE CONSTRUCTION LIMITS ARE BEYOND THE PAVING LIMITS, CRUSHED AGGREGATE SURFACING SHALL BE PLACED BETWEEN THE PAVING LIMITS AND CONSTRUCTION LIMITS.

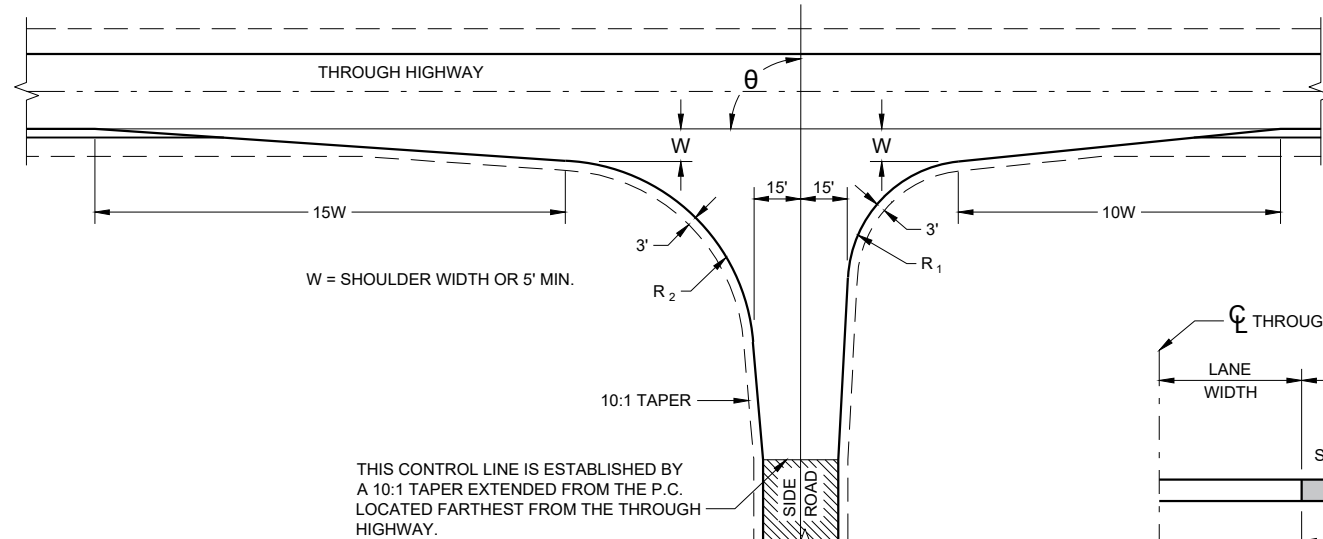
WHEN THE SIDE ROAD IS PRESENTLY PAVED, NEW PAVEMENT SHALL BE PLACED TO THE LIMITS OF DESIGN AS SHOWN AND BEYOND, IF NECESSARY, TO MEET EXISTING PAVEMENT.

WHEN THE SIDE ROAD IS THE CONSTRUCTION PROJECT, THE INTERSECTION SURFACING SHALL BE THE SAME AS FOR THE PROJECT.

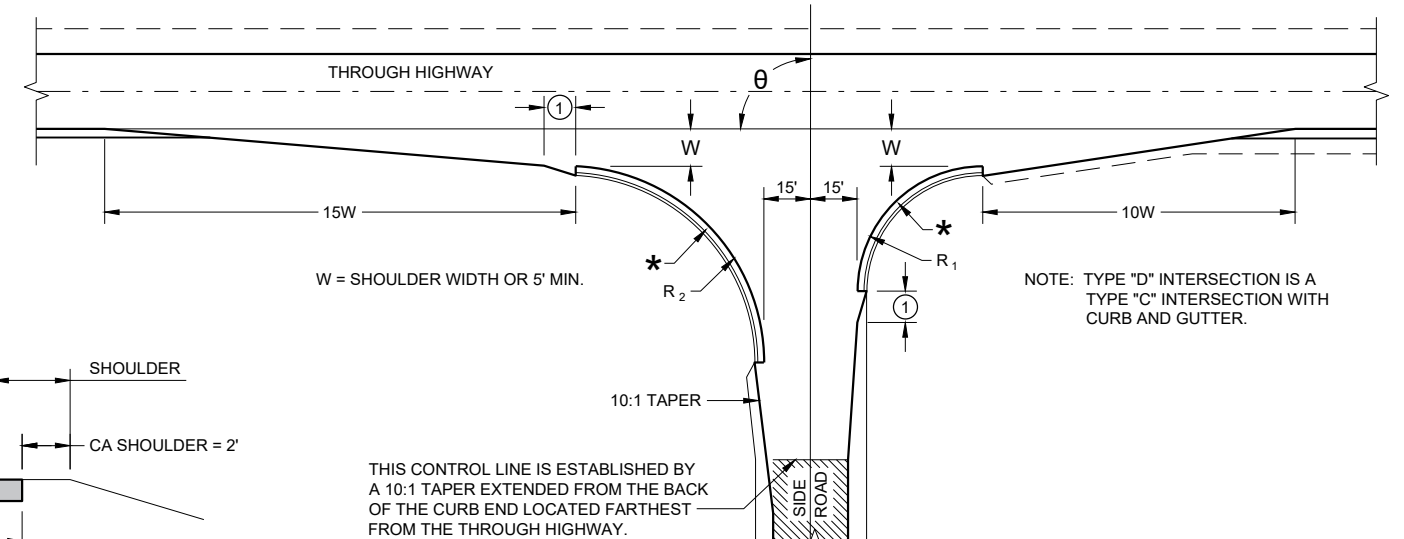
- ① 10-FT TYPICAL.
- ② 12-FT** PLUS ADDITIONAL WIDTH FOR BIKE LANE IF SHOWN ELSEWHERE IN THE PLAN.
**10-FT MAY BE USED ON TYPE B2 ON RESURFACING PROJECTS IF SPECIFIED IN THE CONTRACT.
- ③ BYPASS LANE PAVED SURFACE WIDTH OUTSIDE OF TRAVEL LANE
- ASPHALT = 12-FT PLUS PAVED SHOULDER WIDTH
- PC CONCRETE = 13-FT PLUS PAVED SHOULDER WIDTH
- ④ BYPASS LANE PAVED SHOULDER WIDTH = THE GREATER OF 1-FT OR THE PAVED SHOULDER WIDTH OF THE THROUGH HIGHWAY.

* CONCRETE CURB AND GUTTER 36". TAPER CURB HEIGHT 0" TO 6" IN 10' - 0" LENGTH AT ENDS OF CURB AND GUTTER SECTIONS. WHEN SPECIFIED ELSEWHERE IN THE CONTRACT, PROVIDE CURB OPENING AND FLUME

THIS CONTROL LINE IS ESTABLISHED BY A 10:1 TAPER EXTENDED FROM THE BACK OF THE CURB END LOCATED FARTHEST FROM THE THROUGH HIGHWAY.

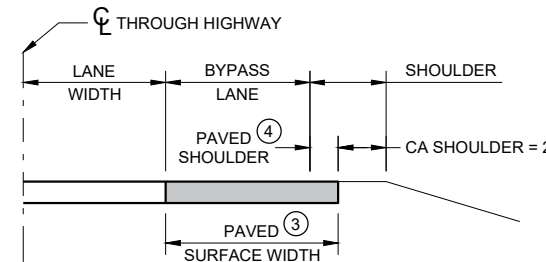


TYPE "C"

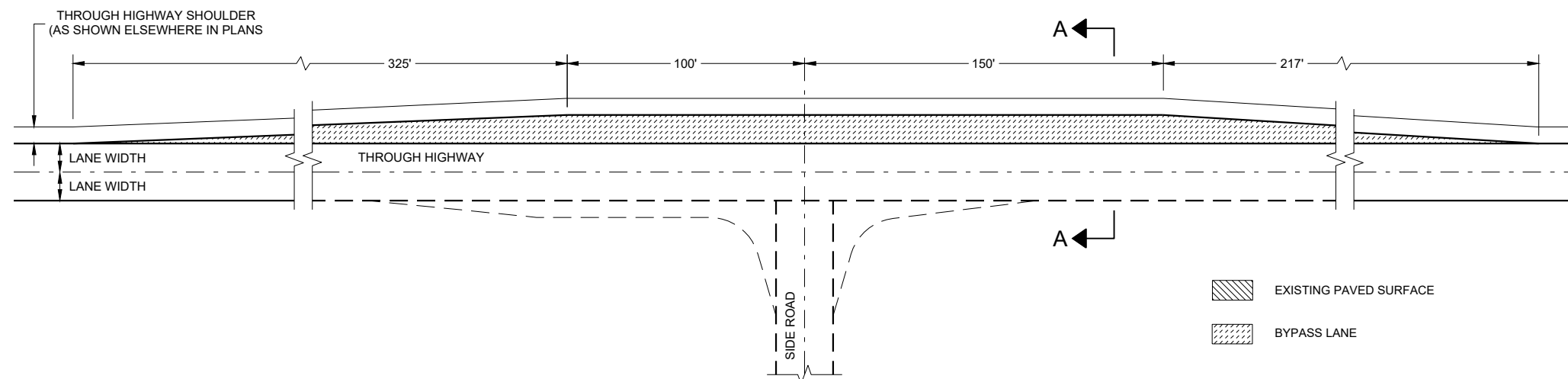


TYPE "D"

NOTE: TYPE "D" INTERSECTION IS A TYPE "C" INTERSECTION WITH CURB AND GUTTER.



SECTION A - A
(SHOWING BYPASS LANE AND SHOULDER)

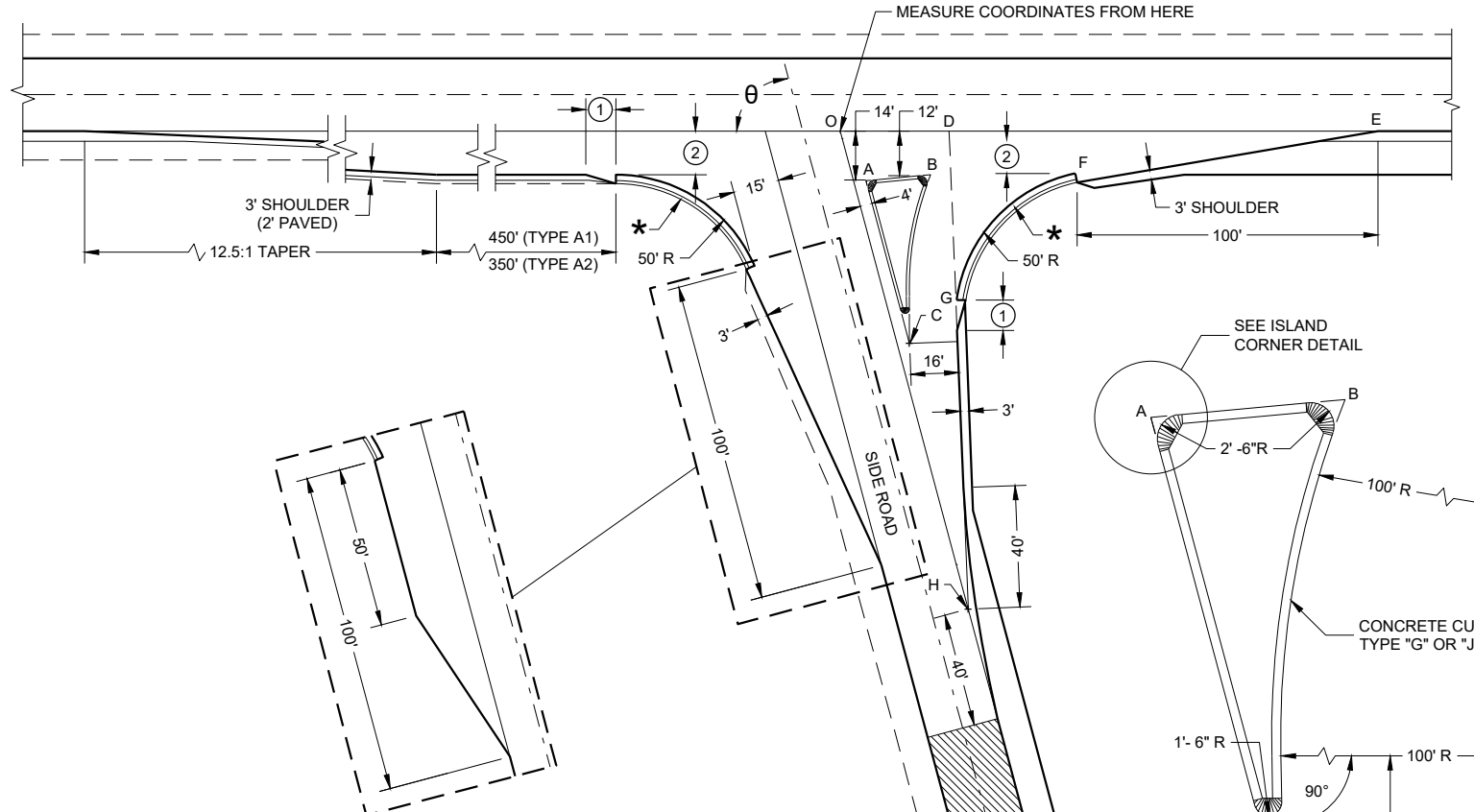
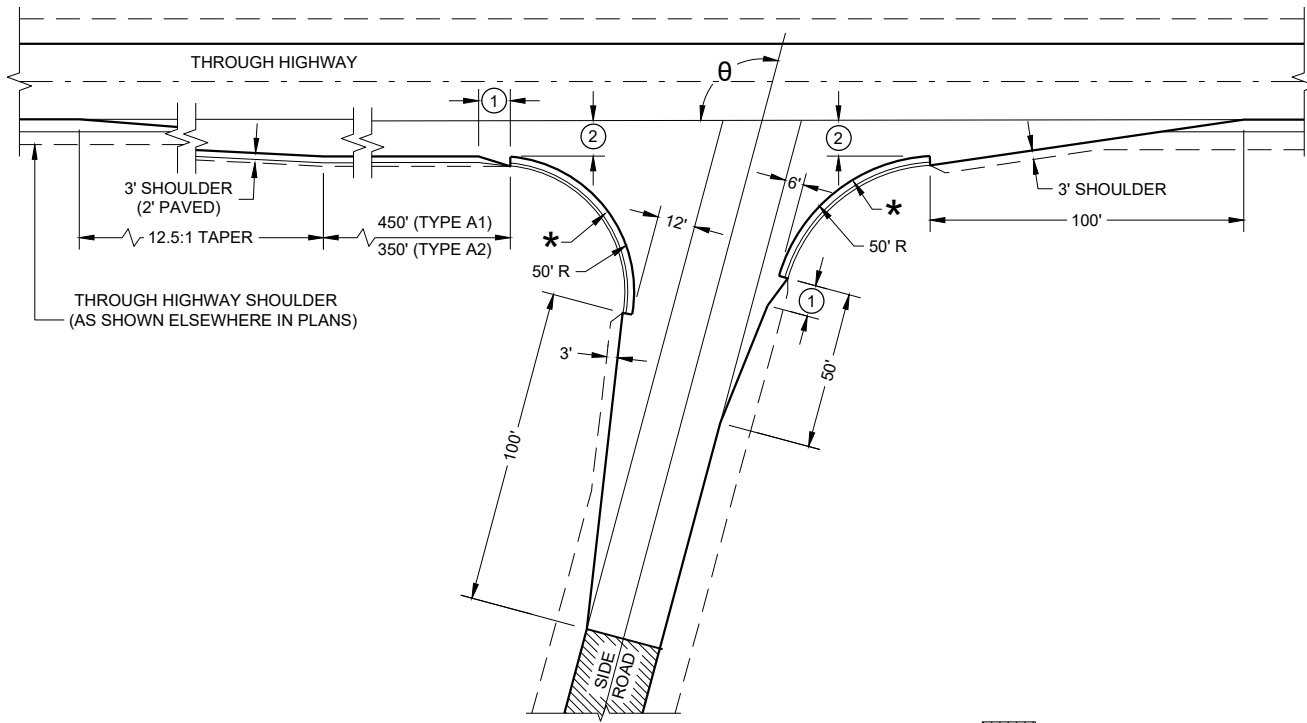


TEE INTERSECTION BYPASS LANE DETAIL

EXISTING PAVED SURFACE
BYPASS LANE

AT GRADE SIDE ROAD INTERSECTION TYPES "B1", "B2", "C", "D" AND TEE INTERSECTION BYPASS LANE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

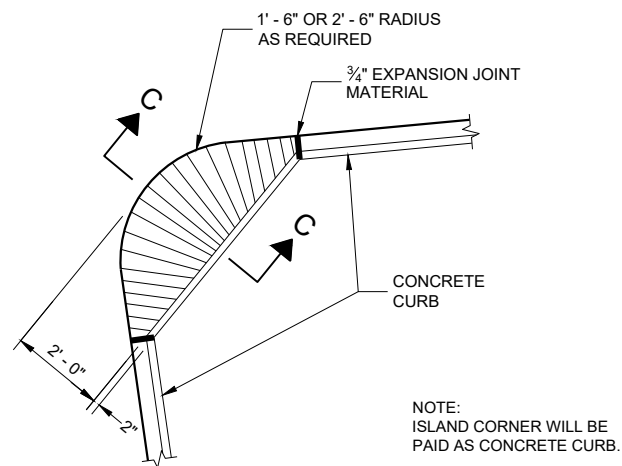


$\theta = \text{MORE THAN } 80^\circ$

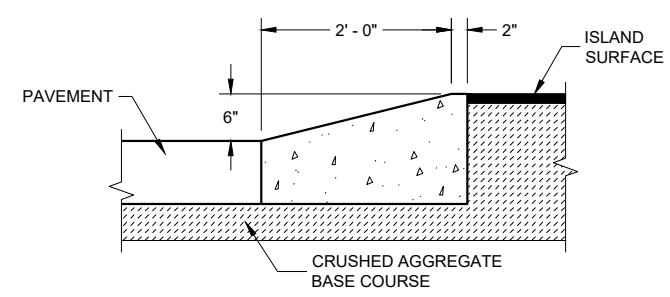
$\theta = \text{ACUTE ANGLES } 80^\circ \text{ OR LESS}$

SIDE ROAD WIDENING AND TAPER REQUIRED WHERE THE THROUGH HIGHWAY CARRIES TWO-WAY TRAFFIC
 $\theta = \text{ACUTE ANGLES } 70^\circ \text{ OR LESS}$

EXISTING PAVED SURFACE



PLAN VIEW



SECTION C - C

ISLAND CORNER DETAIL
 (TO BE CONSTRUCTED AT ALL ISLAND CORNERS)

TABLE OF DIMENSIONS FOR VARIABLE SIDE ROAD INTERSECTION ANGLES
 (INTERPOLATE VALUES FOR ANGLES NOT SHOWN)

ANGLE θ DEGREES	COORDINATES IN FEET (MEASURED FROM POINT "O")								LENGTH IN FEET				
	A	B	C	D	E	F	G	H	AB	AC	T	OJ	OH
60	12.7 -14.0	44.9 -12.0	46.4 -72.4	41.9 0.0	205.0 0.0	104.6 -12.0	64.0 -75.5	85.0 -147.1	32.3	67.4	4.9	85.9	169.9
65	10.9 -14.0	39.0 -12.0	37.8 -71.6	39.4 0.0	196.1 0.0	95.7 -12.0	54.1 -71.5	70.5 -151.3	28.2	63.6	8.5	80.9	166.9
70	9.4 -14.0	33.9 -12.0	29.8 -70.1	37.4 0.0	188.3 0.0	87.8 -12.0	45.6 -67.5	56.1 -154.2	24.6	59.7	11.5	76.1	164.1
75	7.9 -14.0	29.3 -12.0	22.3 -67.9	35.7 0.0	181.2 0.0	80.7 -12.0	38.2 -63.4	41.8 -155.9	21.5	55.8	13.8	71.4	161.4
80	6.5 -14.0	25.4 -12.0	15.6 -65.2	34.4 0.0	174.8 0.0	74.4 -12.0	31.8 -59.3	27.6 -156.5	18.9	52.0	15.6	66.9	158.9

TYPE 'A1" AND "A2" SIDE ROAD INTERSECTION DETAILS

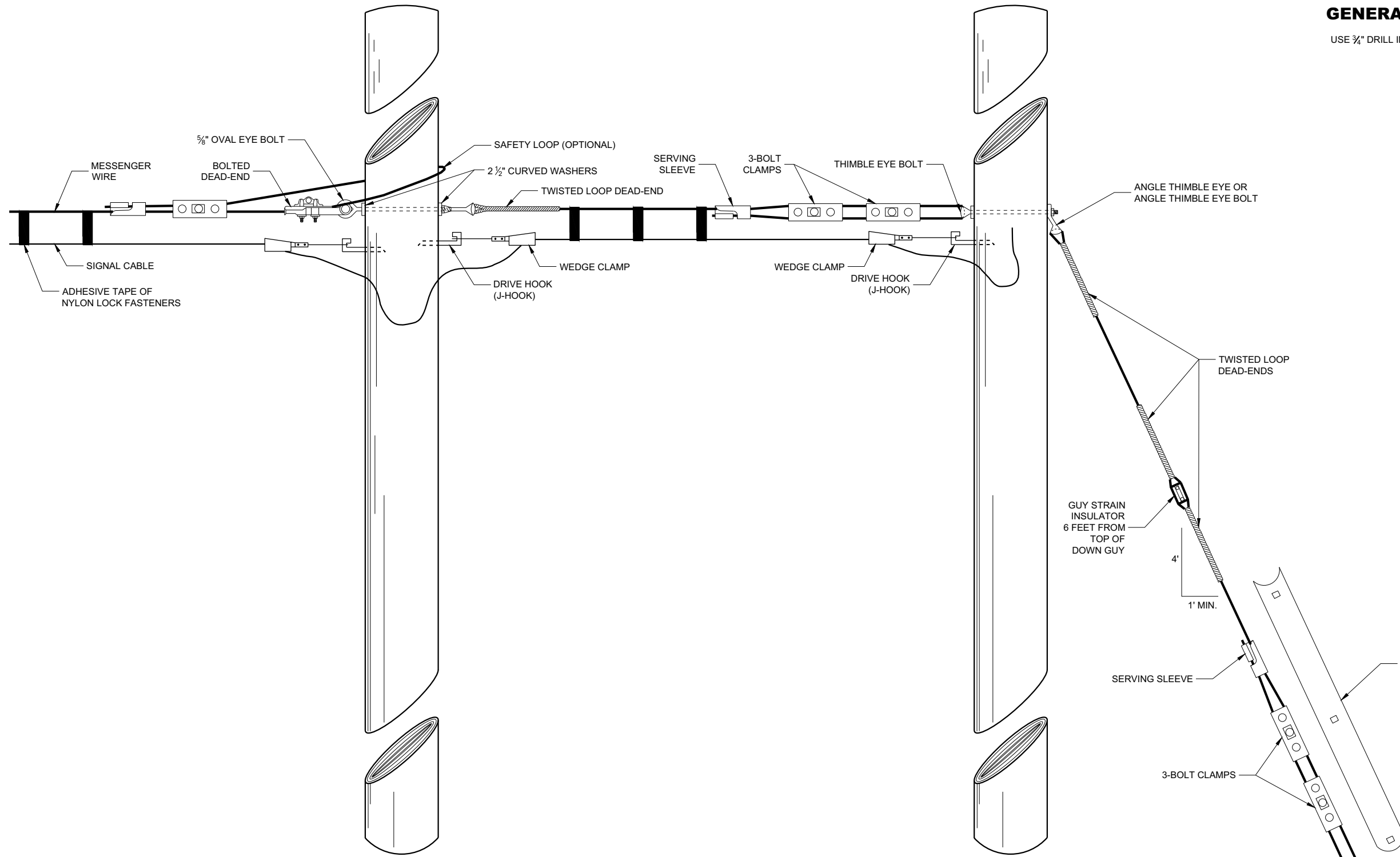
**AT GRADE SIDE ROAD INTERSECTIONS
 TYPES "A1" AND "A2"**

STATE OF WISCONSIN
 DEPARTMENT OF TRANSPORTATION

APPROVED
 November 2022 /S/ John Jenkins
 DATE ROADWAY STANDARDS DEVELOPMENT
 ENGINEER
 FHWA

GENERAL NOTES

USE 3/4" DRILL IN WOOD POLE TO PROVIDE FOR 5/8" BOLTS.



SPAN WIRE POLE

GUY POLE

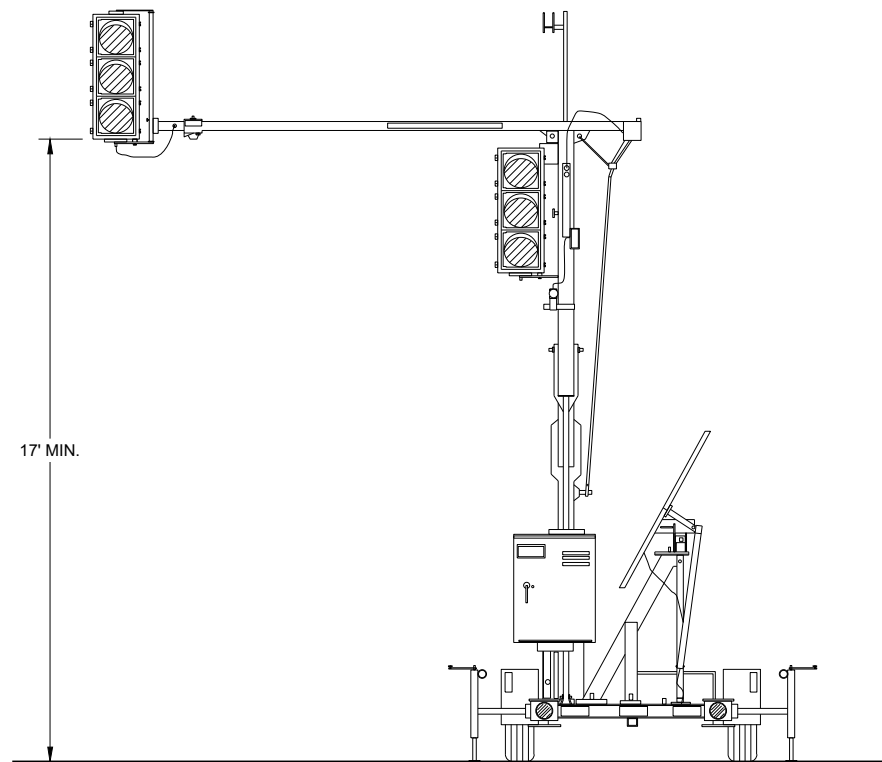
TYPICAL DEAD-ENDINGS OR GUYING

BRIDGE TEMPORARY TRAFFIC SIGNAL INSTALLATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
June 2015 DATE /S/ Ahmet Demerbilek
ROADWAY STANDARDS DEVELOPMENT ENGINEER

FHWA

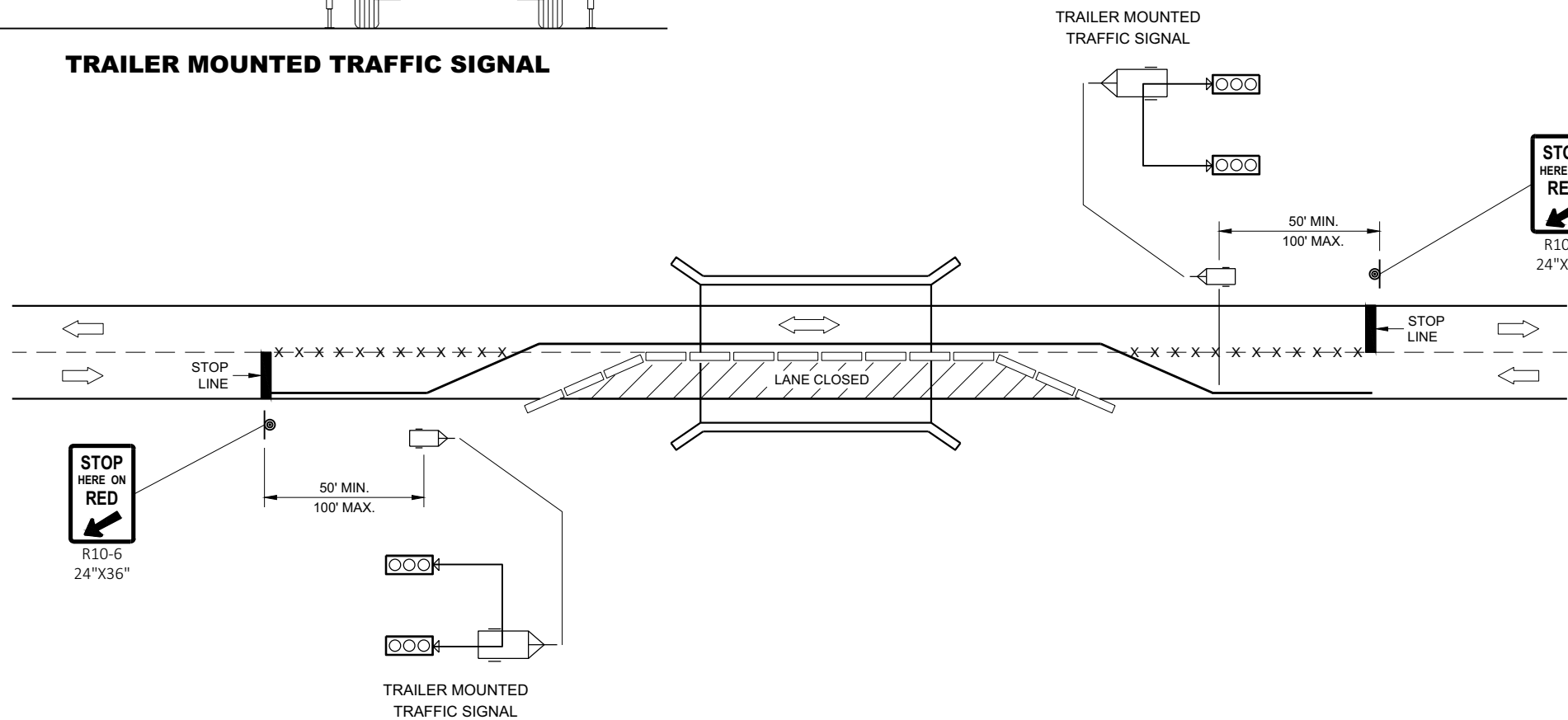


TRAILER MOUNTED TRAFFIC SIGNAL

GENERAL NOTES






DETAIL OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

SIGNING, PAVEMENT MARKING AND LANE CONTROL REQUIREMENTS SHALL CONFORM TO STANDARD DETAIL DRAWING 15D33.



TYPICAL TRAILER MOUNTED TRAFFIC SIGNAL LOCATION

LEGEND

-  POST MOUNTED SIGN
-  TEMPORARY PRECAST CONCRETE BARRIER
-  TRAILER MOUNTED TRAFFIC SIGNAL
-  REMOVE PAVEMENT MARKINGS
-  DIRECTION OF TRAFFIC

BRIDGE TEMPORARY TRAFFIC SIGNAL INSTALLATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

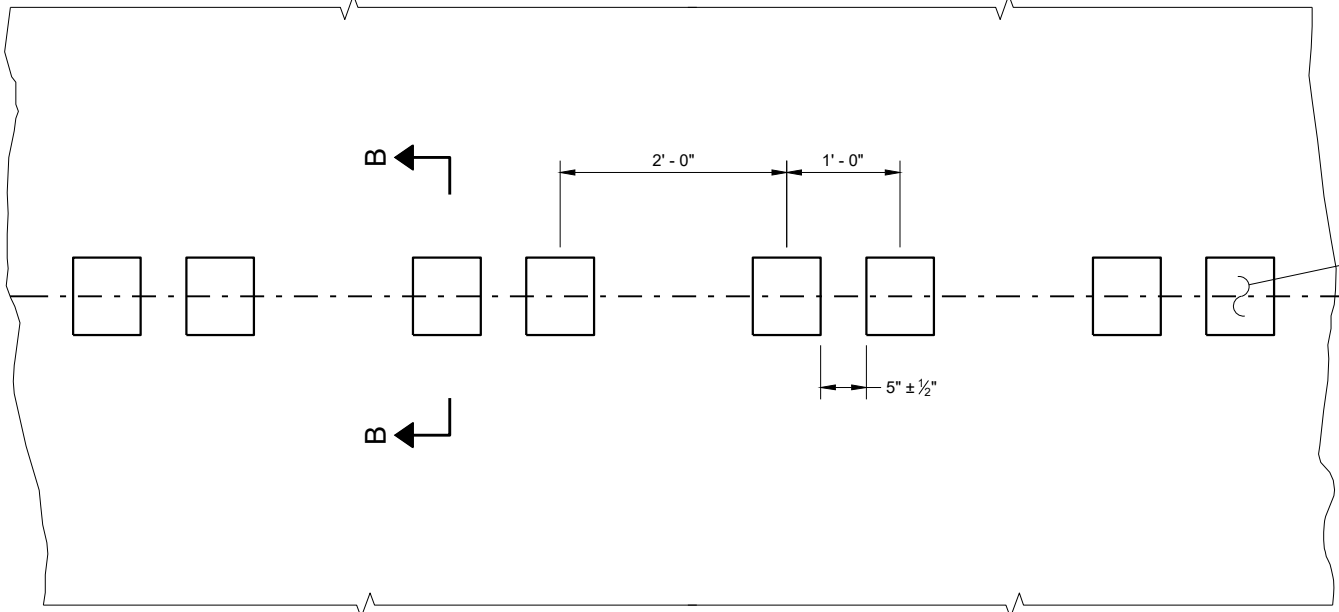
APPROVED
June 2015 /S/ Ahmet Demerbilek
DATE ROADWAY STANDARDS DEVELOPMENT ENGINEER

FHWA

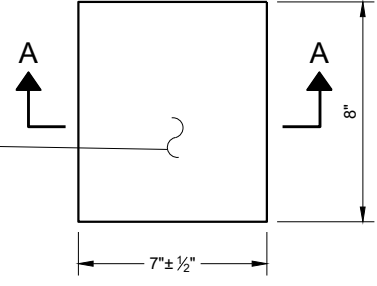
GENERAL NOTES

DO NOT MILL SHOULDER GROOVES THROUGH INTERSECTIONS, MARKED CROSSWALKS, NON-MOTORIZED PATH CROSSINGS, ETC. REFER TO SDD 13A11 SHEETS "d" AND "e".

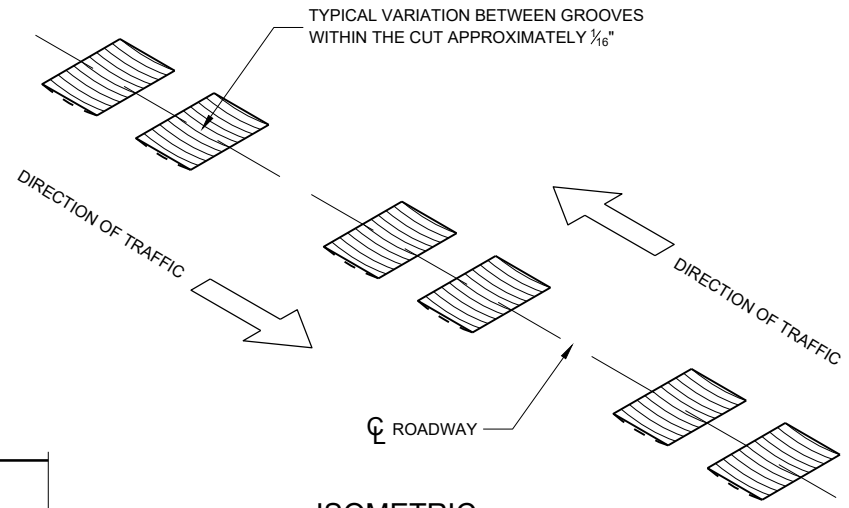
CENTERLINE GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS WHEN DIRECTED BY THE ENGINEER.



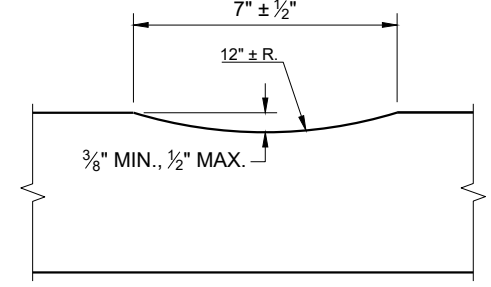
PLAN DETAIL VIEW



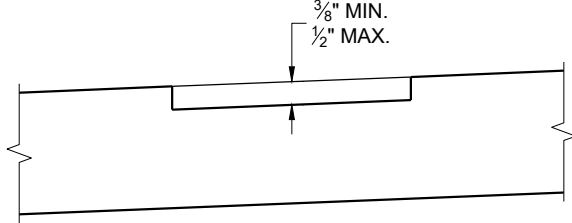
PLAN VIEW (SINGLE GROOVE)



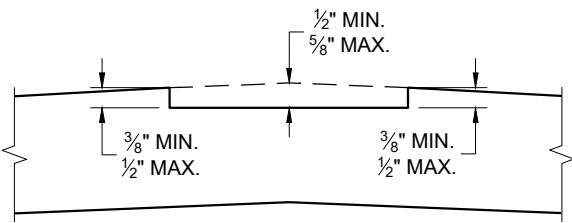
ISOMETRIC



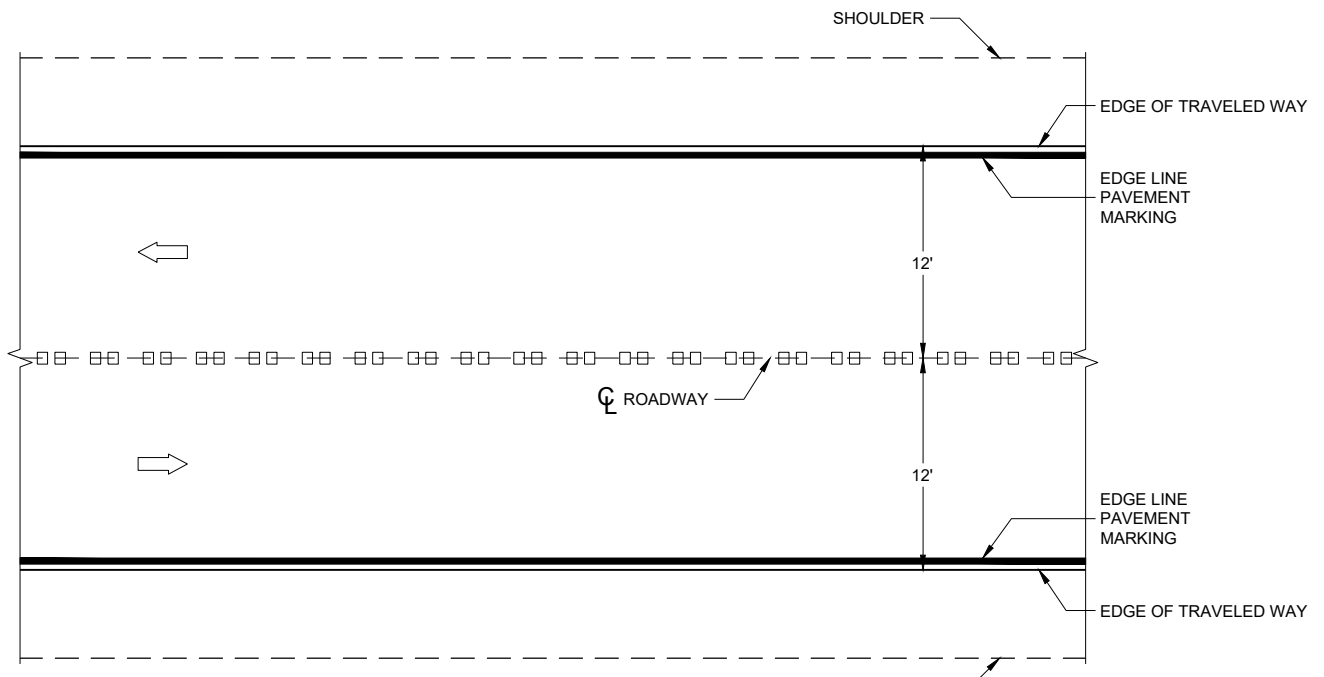
SECTION A - A



SECTION B - B SUPERELEVATED ROADWAY



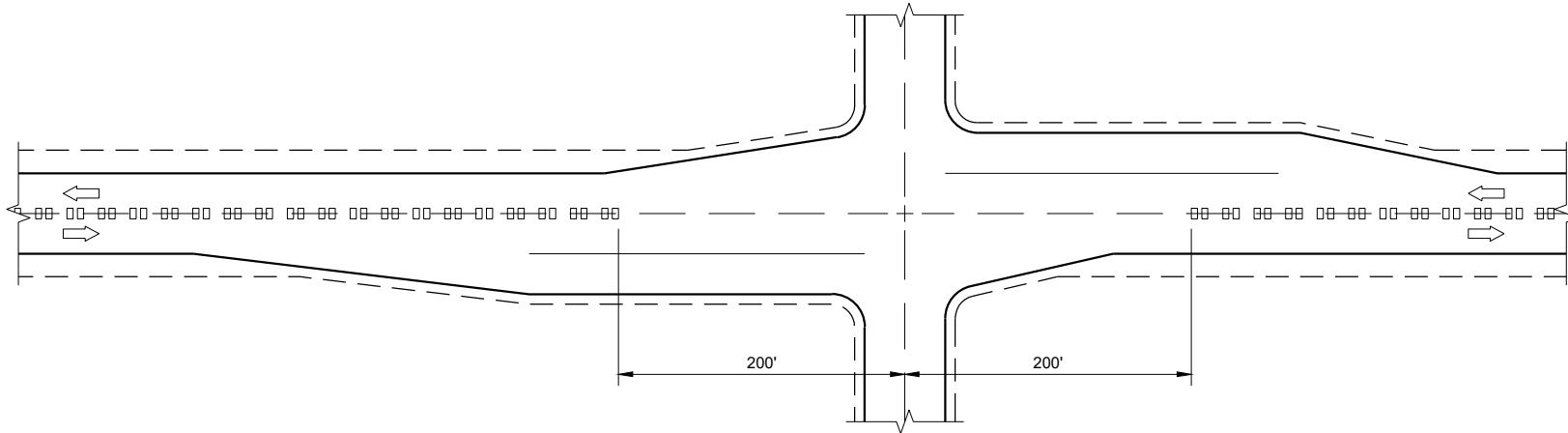
SECTION B - B CROWNED ROADWAY



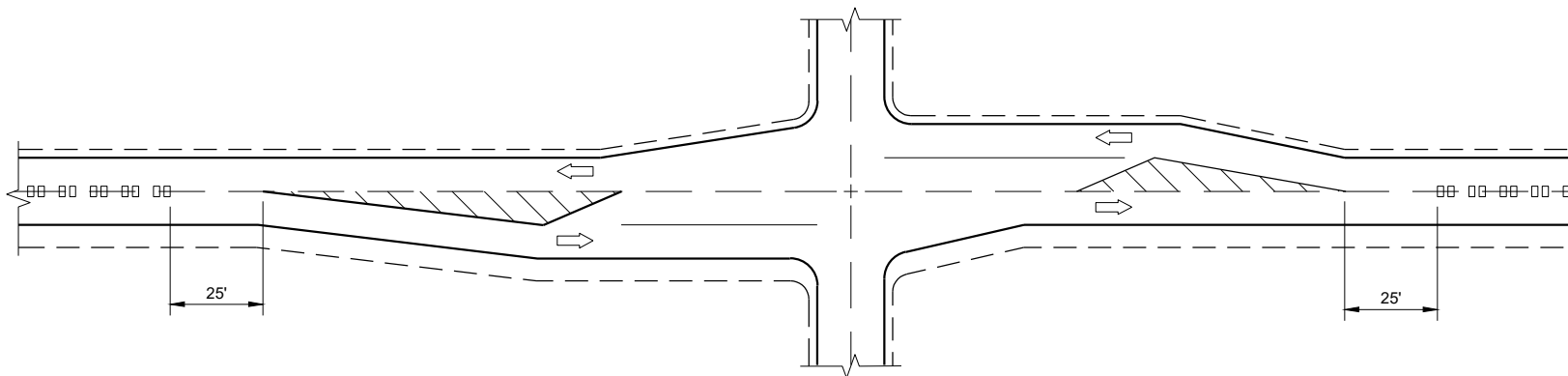
PLAN VIEW

CENTERLINE RUMBLE STRIPS - ASPHALT

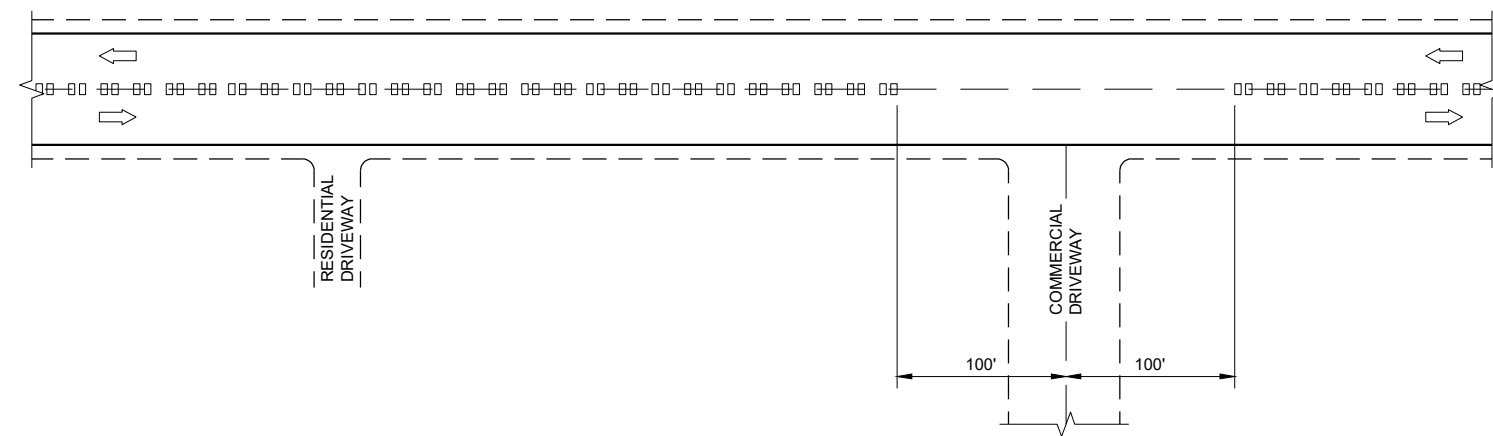
CENTERLINE RUMBLE STRIPS - ASPHALT
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION



CENTERLINE GROOVES AT INTERSECTIONS



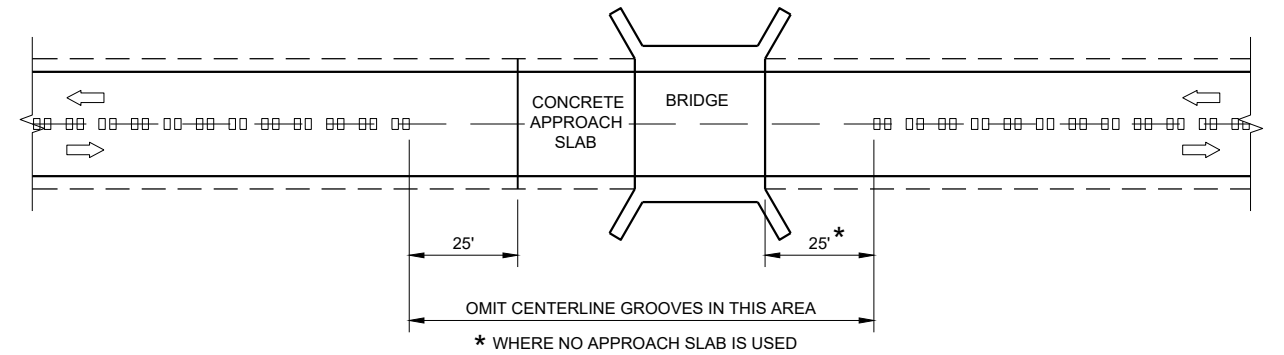
**CENTERLINE GROOVES AT INTERSECTIONS
(WITH LEFT TURN LANES)**



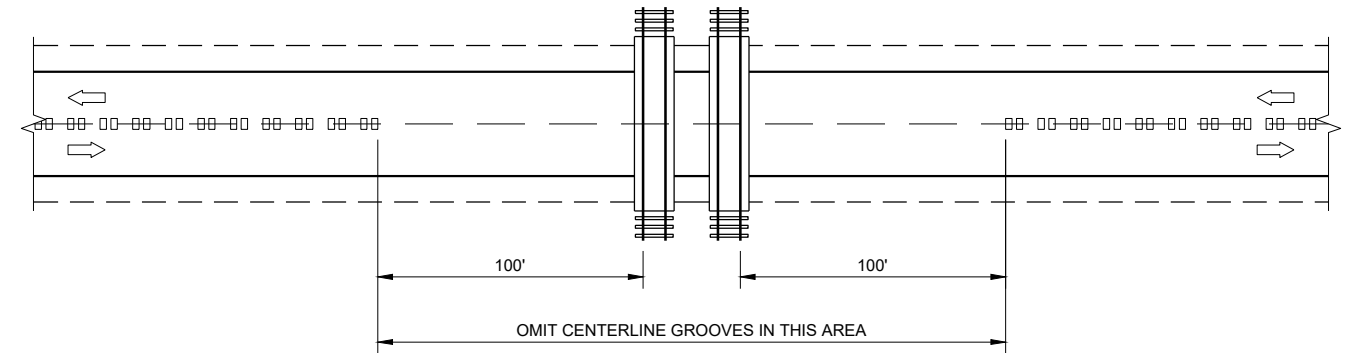
CENTERLINE GROOVES AT DRIVEWAYS^①

GENERAL NOTES

① CENTERLINE GROOVES MAY BE OMITTED IN AREAS WITH HIGH CONCENTRATIONS OF DRIVEWAYS WHEN DIRECTED BY THE ENGINEER.



CENTERLINE GROOVES AT BRIDGES



CENTERLINE GROOVES AT RAILROADS

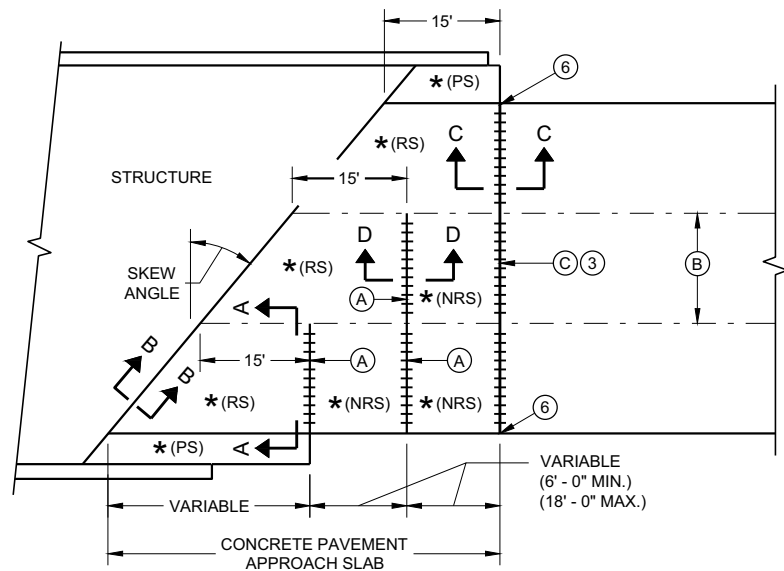
6

6

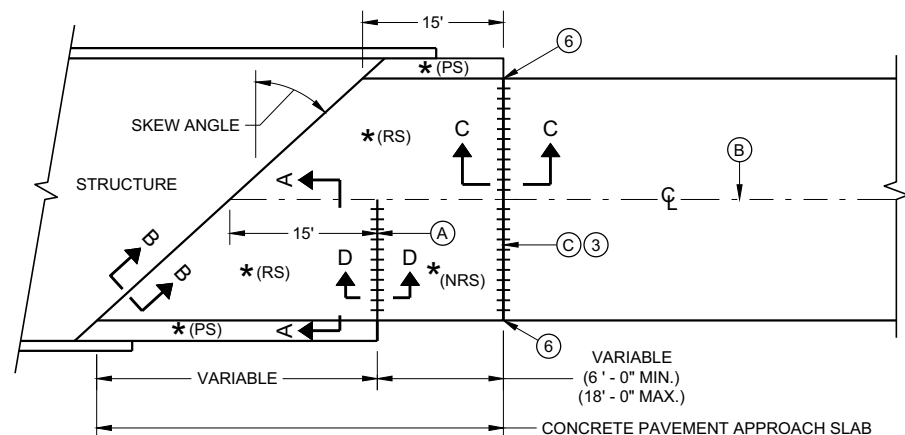
SDD 13A11 - 04d

SDD 13A11 - 04d

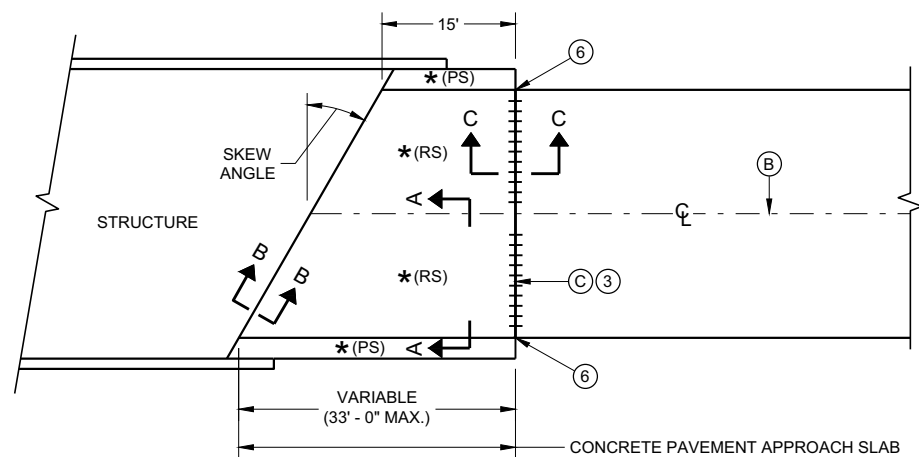
CENTER LINE RUMBLE STRIPS - INTERSECTIONS, DRIVEWAYS, BRIDGES, RAIL ROADS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2023 DATE	/S/ John Jenkins ROADWAY STANDARDS DEVELOPMENT ENGINEER
FHWA	



**SKewed Approach
(Pavement more than two lanes)**

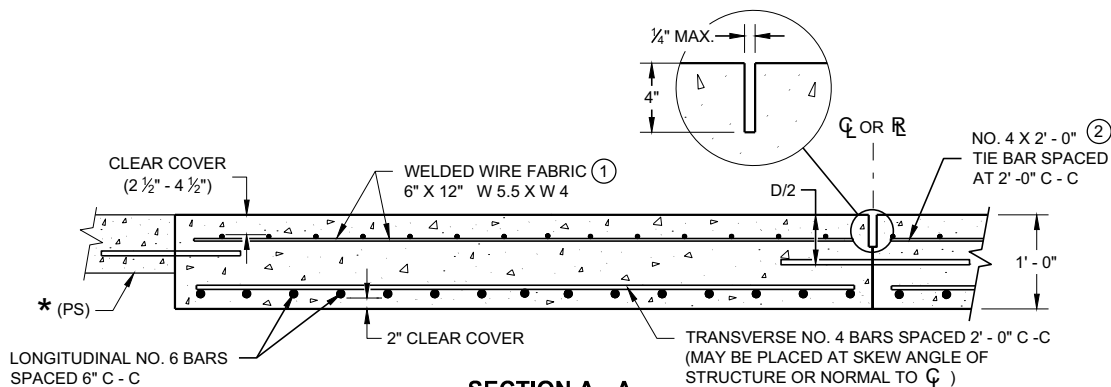


**Skews > 20°
(Pavement width ≤ 30')**

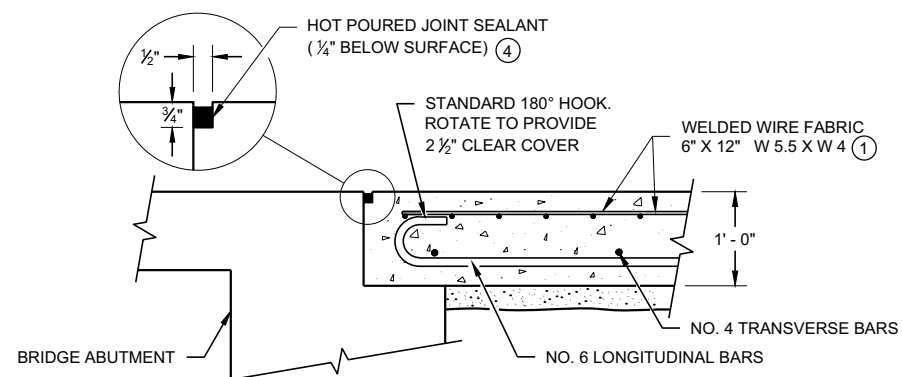


**Skews ≤ 20°
(Pavement width ≤ 30')**
Approach Slab and Adjacent Pavement

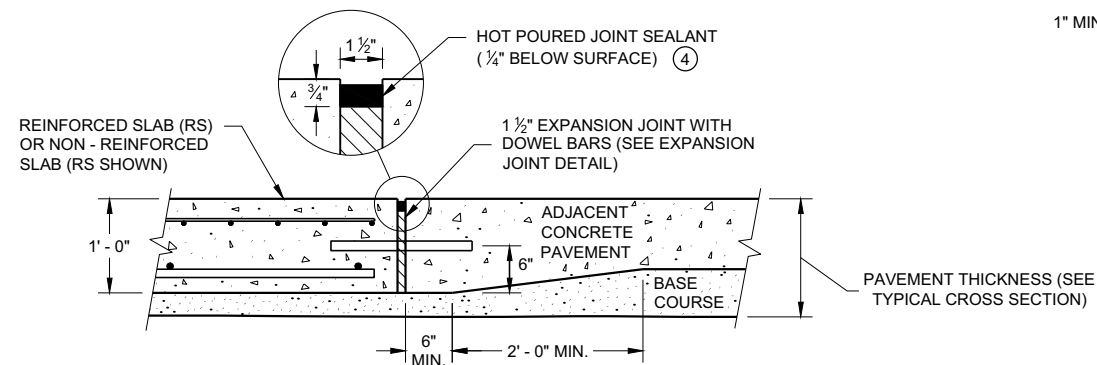
- * (RS) = REINFORCED CONCRETE SLAB
- * (PS) = PAVED CONCRETE SHOULDER OR CONCRETE DRAINAGE SLAB
- * (NRS) = NON - REINFORCED CONCRETE SLAB
- *** STANDARD DOWEL BAR DIAMETER (SEE SDD 13C11 AND SDD 13C13)



**SECTION A - A
REINFORCEMENT POSITIONING DETAIL**



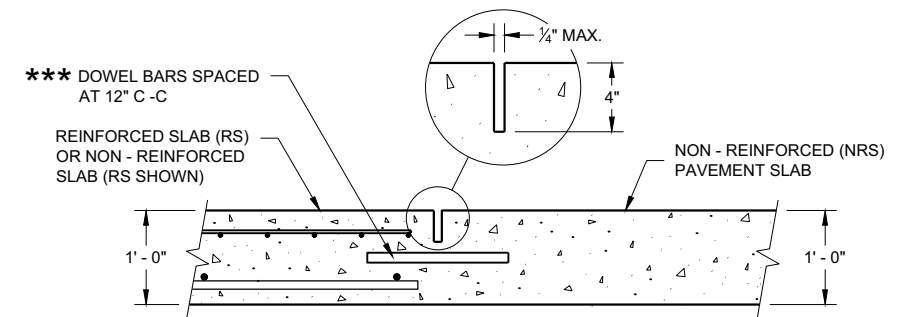
**SECTION B - B
BEND DETAIL
BOTTOM REINFORCEMENT**



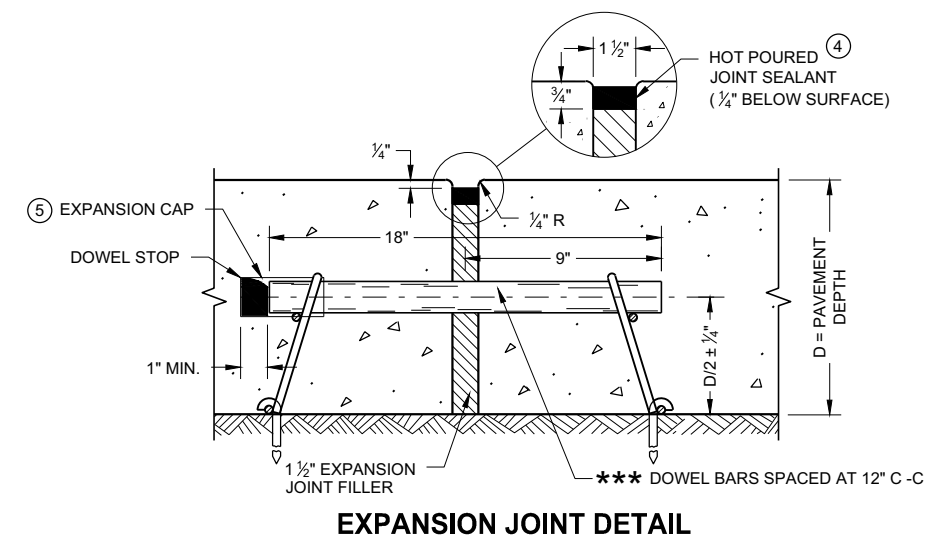
**SECTION C - C
TRANSITION DETAIL
APPROACH SLAB TO ADJACENT PAVEMENT**

GENERAL NOTES

- THE CONTRACTOR MAY SPLICE NO. 6 BARS IN THE APPROACH SLAB FOR SKEWED STRUCTURES ONLY. STAGGER SPLICES WITH A MAXIMUM OF ONE SPLICE PER BAR. THE LENGTH OF LAP IS 20 INCHES.
- TACK WELD DOWEL BARS TO THE BASKETS ON ALTERNATE ENDS.
- ① THE CONTRACTOR MAY USE NO. 4 BARS SPACED AT 2' - 0" C - C IN BOTH THE LONGITUDINAL AND TRANSVERSE DIRECTIONS FOR TOP REINFORCEMENT AS AN ALTERNATIVE TO THE WELDED WIRE FABRIC.
 - ② THE CONTRACTOR MAY OMIT THE BARS BETWEEN REINFORCED SLABS WHERE SLAB REINFORCEMENT BARS EXTEND ACROSS THE CENTERLINE OR REFERENCE LINE.
 - ③ DO NOT CONSTRUCT AN EXPANSION JOINT OR INSTALL DOWEL BARS WHEN ABUTTING AN HMA PAVEMENT.
 - ④ USE A JOINT SEALANT CONFORMING TO STANDARD SPECIFICATION 415.2.6.
 - ⑤ PLACE EXPANSION CAP ON THE END OF THE DOWEL THAT IS NOT TACK WELDED TO THE BASKET. DO NOT FORCE DOWEL BAR PAST THE DOWEL STOP.
 - ⑥ EXTEND EXPANSION JOINT THROUGH ANY ADJACENT TIED CONCRETE.
 - (A) STANDARD CONTRACTION JOINT NORMAL TO C OR R.
 - (B) STANDARD LONGITUDINAL JOINT WITH TIE BARS.
 - (C) 1 1/2" EXPANSION JOINT WITH DOWEL BARS NORMAL TO C OR R.



**SECTION D - D
CONTRACTION JOINT**



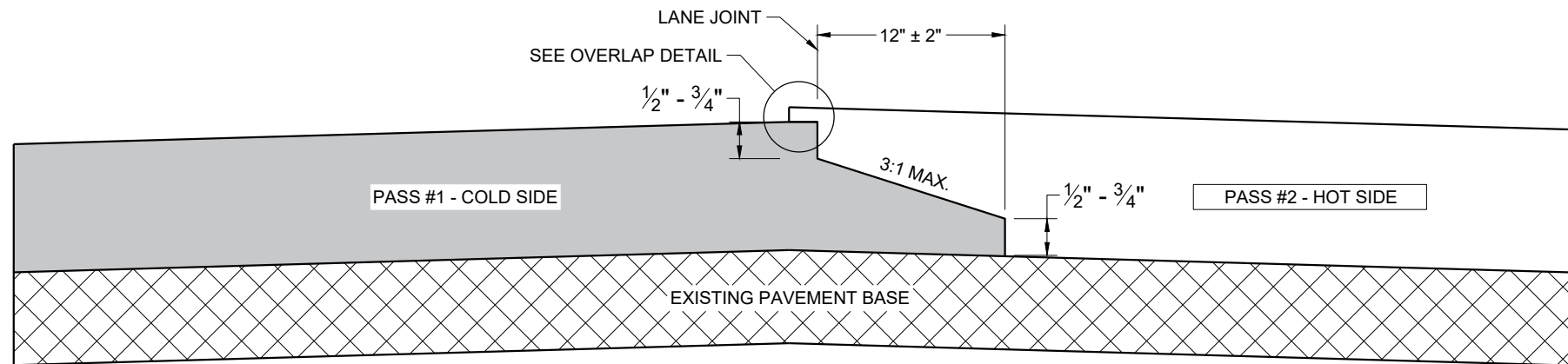
EXPANSION JOINT DETAIL

**CONCRETE PAVEMENT
APPROACH SLAB**

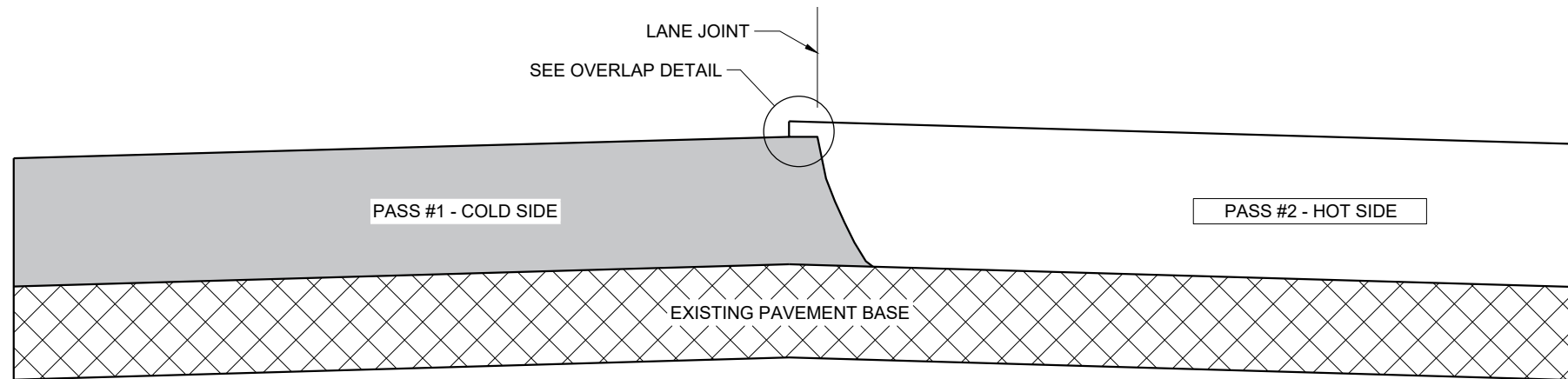
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2018 /S/ Peter Kemp, P.E.
DATE DATE PAVEMENT SUPERVISOR

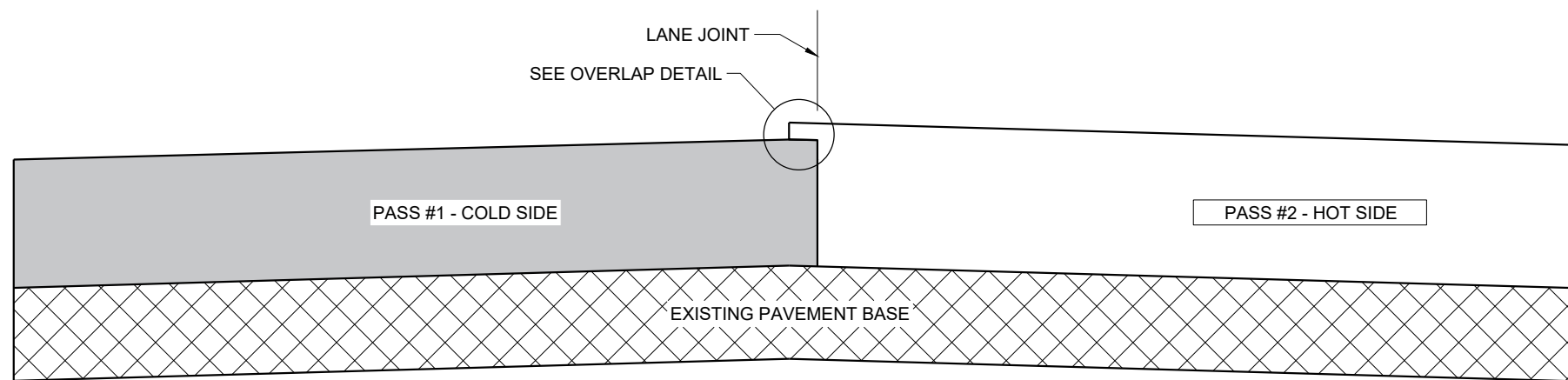
FHWA



**TYPICAL PAVEMENT CROSS SECTION
NOTCHED WEDGE JOINT**



**TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT**



**TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT (MILLED)**

GENERAL NOTES

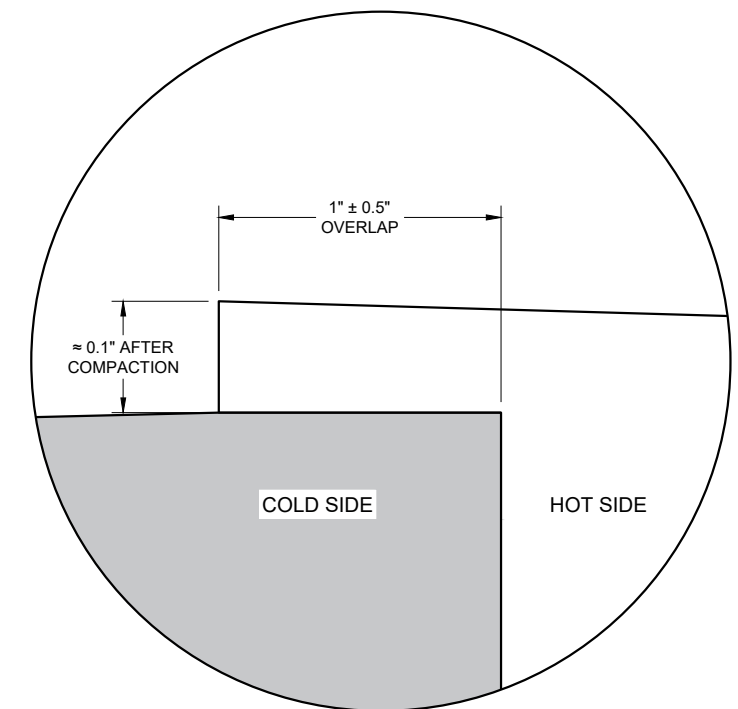
IN ADDITION TO THE DETAILS PROVIDED IN THIS DRAWING, CONFORM TO STANDARD SPECIFICATION 450.3.2.8 FOR WHEN A NOTCHED WEDGE JOINT IS REQUIRED AND FOR GENERAL JOINT CONSTRUCTION REQUIREMENTS.

FOR ALL LONGITUDINAL JOINTS, ENSURE THE PAVER SCREED OVERLAPS THE PREVIOUSLY PLACED PAVEMENT BY $1" \pm 0.5"$ AND THE HOT SIDE OF THE JOINT REMAINS HIGHER THAN THE COLD SIDE BY APPROXIMATELY $0.1"$ AFTER FINAL COMPACTION. (IT WILL BE FLUSH WHEN PAVING IN ECHELON.)

ONLY REMOVE THE LONGITUDINAL NOTCHED WEDGE JOINT FOR SMA PAVEMENT OR AS DIRECTED BY THE ENGINEER TO ADDRESS SPECIFIC LENGTHS OF JOINT DAMAGED BY TRAFFIC.

WHEN MILLING BACK OR REMOVING ANY LONGITUDINAL JOINT, LIMIT THE MATERIAL REMOVED TO $2"$ FROM THE TOP NOTCH OR FROM THE VERTICAL JOINT EDGE ON THE COLD SIDE OF THE JOINT.

USE LONGITUDINAL MILLED JOINT AS PLANS SHOW OR THE AS THE ENGINEER DIRECTS.



OVERLAP DETAIL (TYPICAL)

6

6

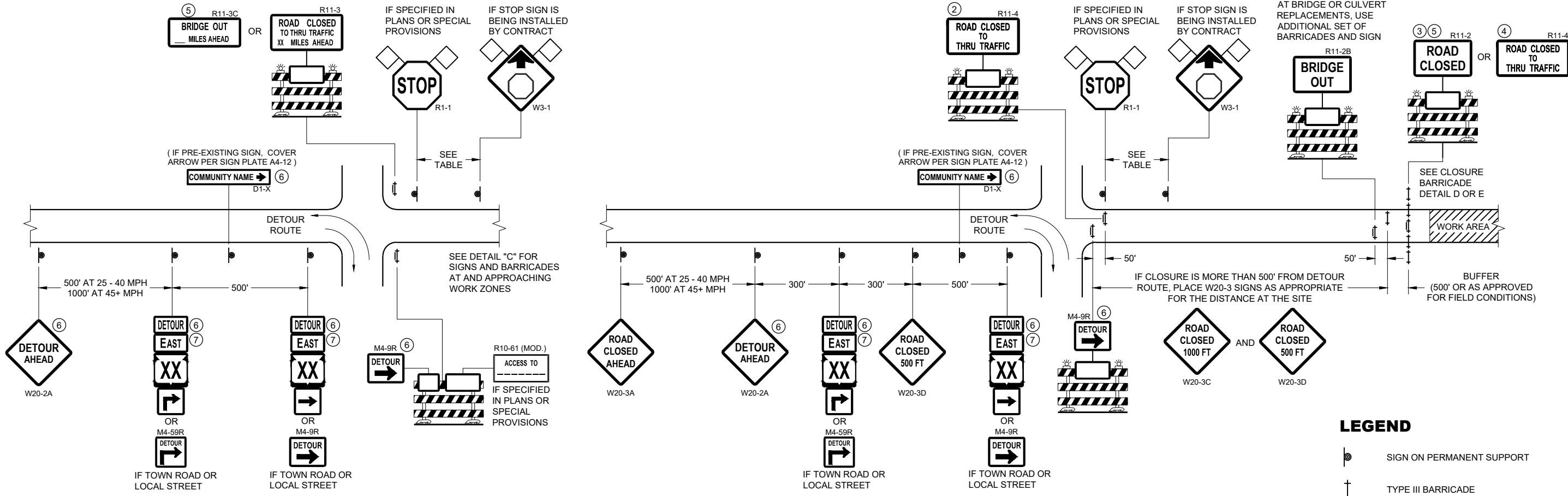
SDD 13C19 - 03

SDD 13C19 - 03

HMA LONGITUDINAL JOINTS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2020 DATE /S/ Steven Hefel
HMA PAVEMENT ENGINEER
FHWA



**DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR**

WORK ZONE GREATER THAN OR EQUAL TO 1/2 MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)

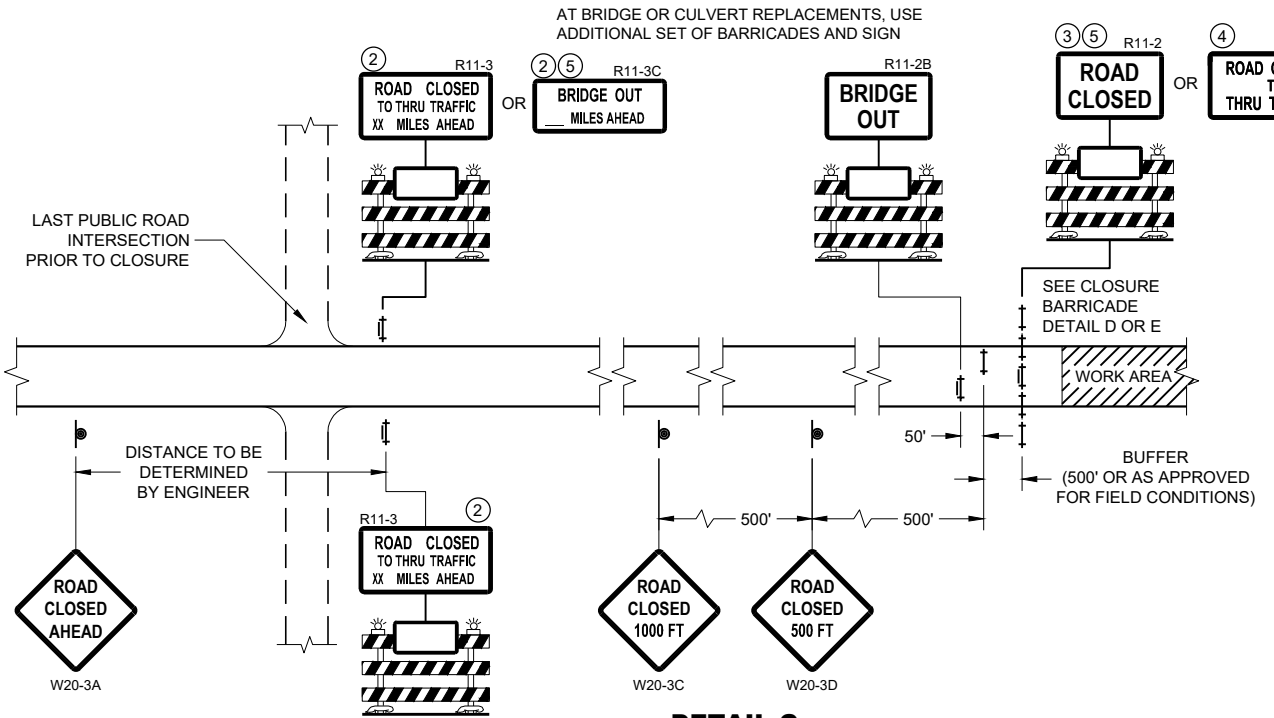
**DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR**

WORK ZONE LESS THAN 1/2 MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)

- LEGEND**
- SIGN ON PERMANENT SUPPORT
 - TYPE III BARRICADE
 - TYPE III BARRICADE WITH ATTACHED SIGN
 - TYPE "A" WARNING LIGHT (FLASHING)
 - WORK AREA
 - FLAGS, 16" X 16" MIN. (ORANGE)

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

- M4 - 8
- M3 - X
- M1 - 4
- M1 - 6
- M1 - 5A
- M05 - 1
- M06 - 1



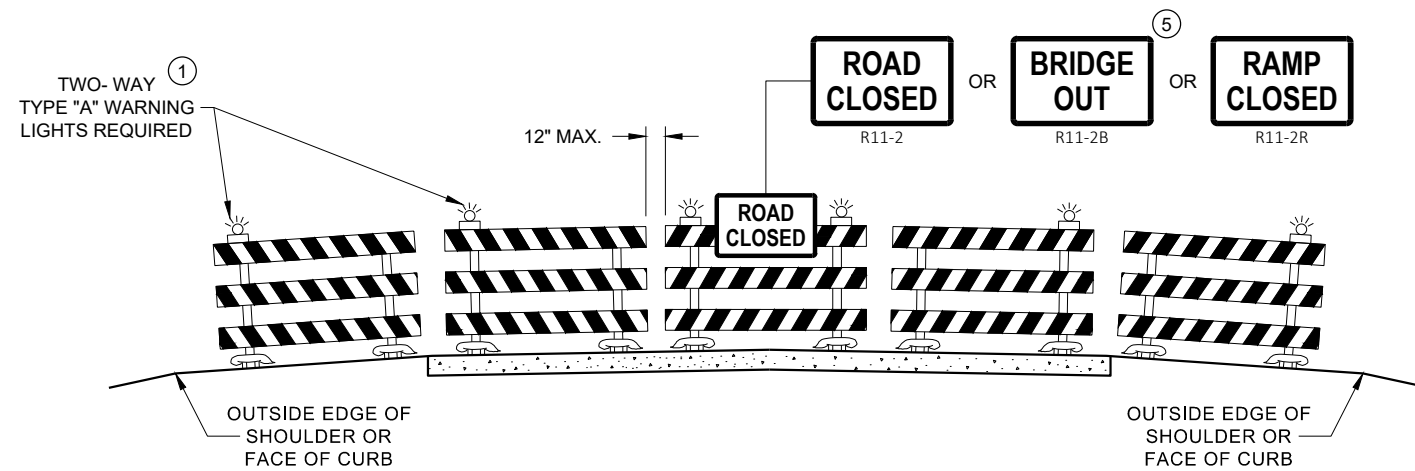
**DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR**

SEE SDD 15C2-SHEET "b"
FOR GENERAL NOTES
AND FOOTNOTES ① THROUGH ⑦

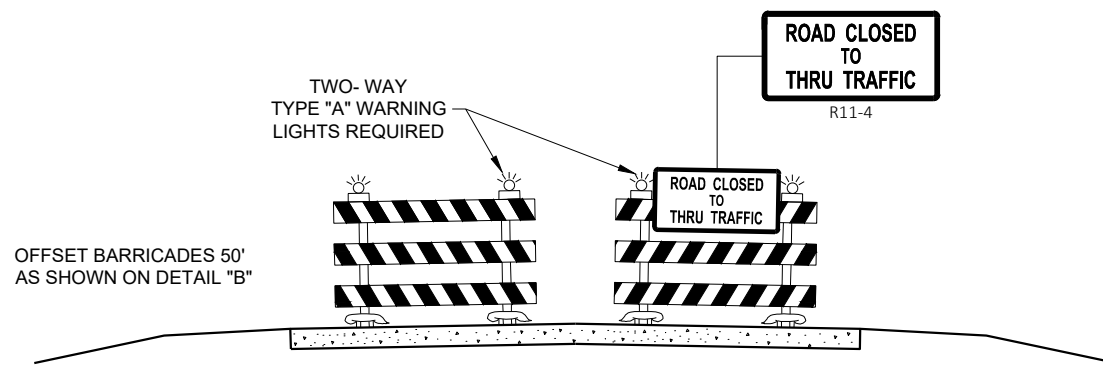
**BARRICADES AND SIGNS
FOR MAINLINE CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 /S/ Andrew Heidtke
DATE DATE WORK ZONE ENGINEER
FHWA



**DETAIL D
ROAD CLOSURE BARRICADE DETAIL
APPROACH VIEW**



**DETAIL E
LANE CLOSURE BARRICADE DETAIL
APPROACH VIEW**

SEE SDD 15C2 - SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW - INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11 - 2, R11 - 3, M4 - 9, R11 - 4, AND R10 - 61 SIGNS PLACED ON THE BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE RAIL OR BOTTOM RAILS.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

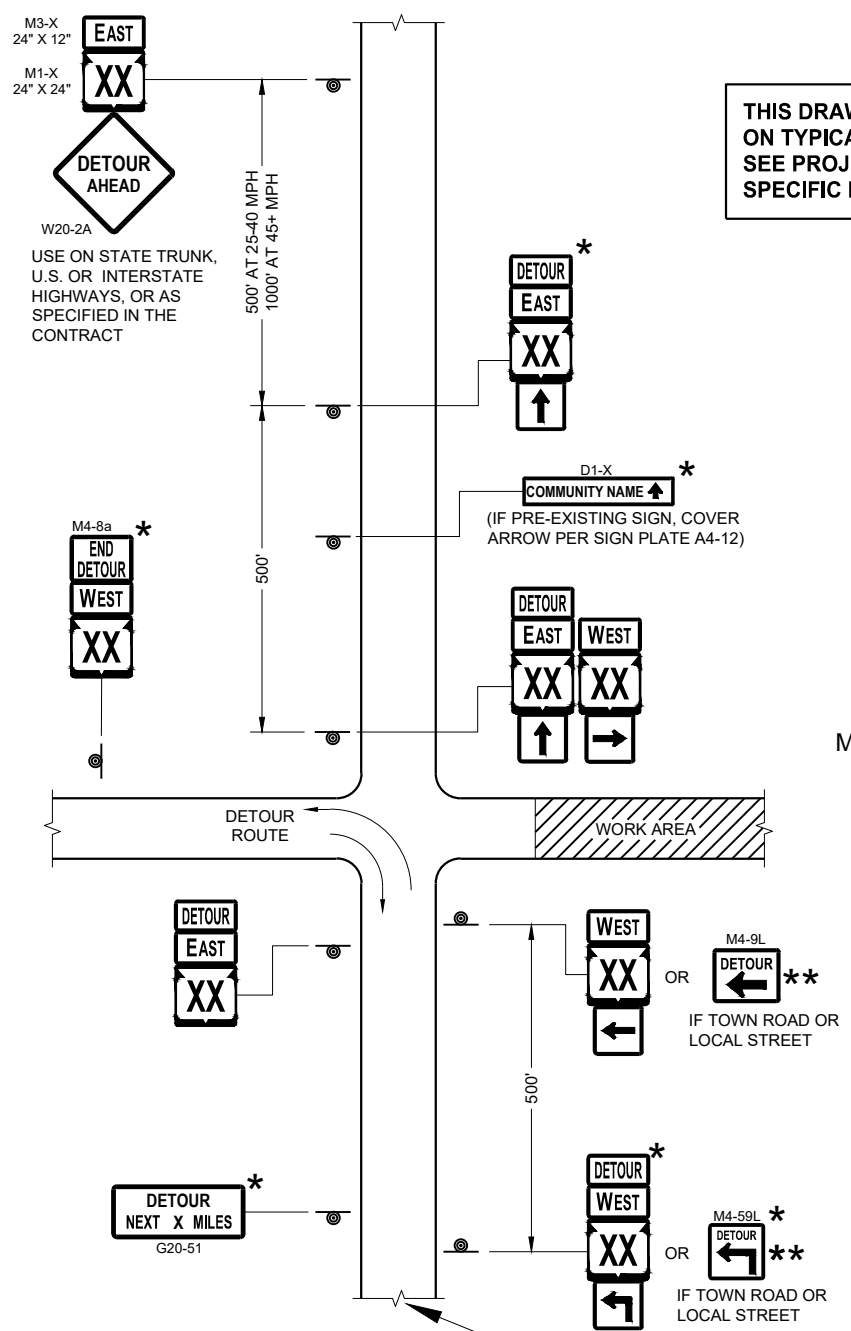
- R11 - 2 SHALL BE 48" X 30"
- R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60" X 30"
- M4 - 9 SHALL BE 30" X 24"
- M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1 - 1 SHALL BE 36" X 36"

- ① TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT SPACING).
- ② THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.
- ③ FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".
- ④ FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".
- ⑤ FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.
- ⑥ INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.
- ⑦ "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

**BARRICADES AND SIGNS
FOR
VARIOUS CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER



THIS DRAWING PROVIDES GENERAL GUIDANCE ON TYPICAL DETOUR SIGN LAYOUT AND SPACING. SEE PROJECT DETOUR SIGNING SHEETS FOR SPECIFIC DETAILS FOR EACH PROJECT.

LEGEND

- SIGN ON PERMANENT SUPPORT
- WORK AREA
- M4 - 8
- M3 - X
- M1 - 4
- M1 - 6
- M1 - 5A
- M05 - 1
- M06 - 1
- M06 - 1

GENERAL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. MODIFY EXISTING SIGNS WHERE POSSIBLE.

THE SPACING BETWEEN TRAFFIC CONTROL AND DETOUR SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

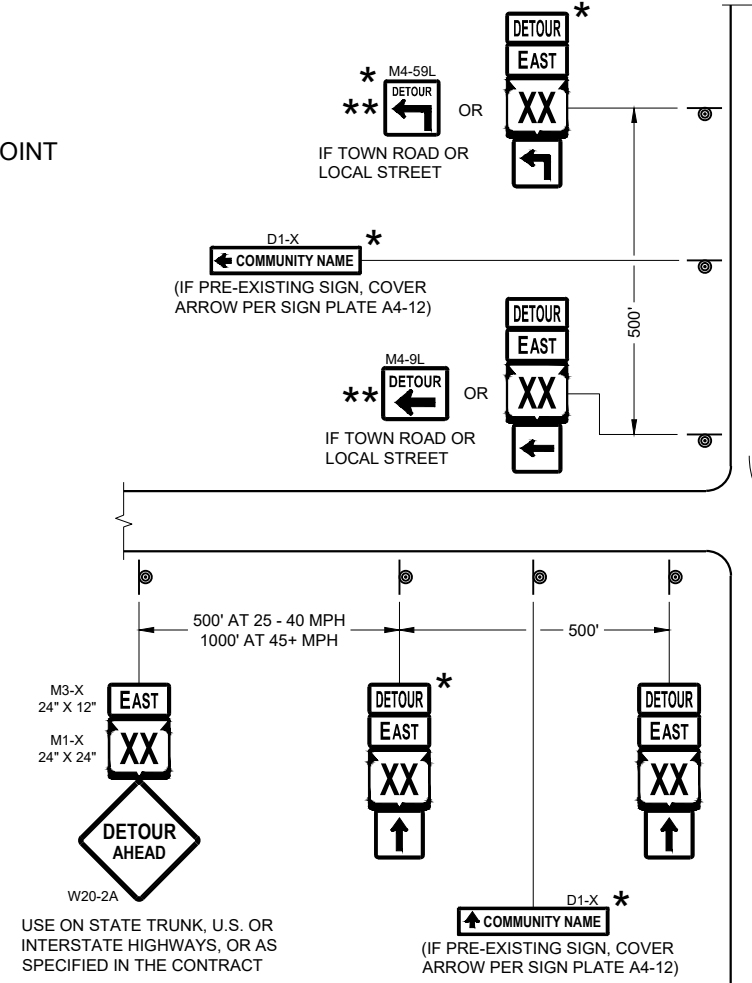
"MO" SIGNS ARE THE SAME AS "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGN SIZES SHALL BE AS FOLLOWS:

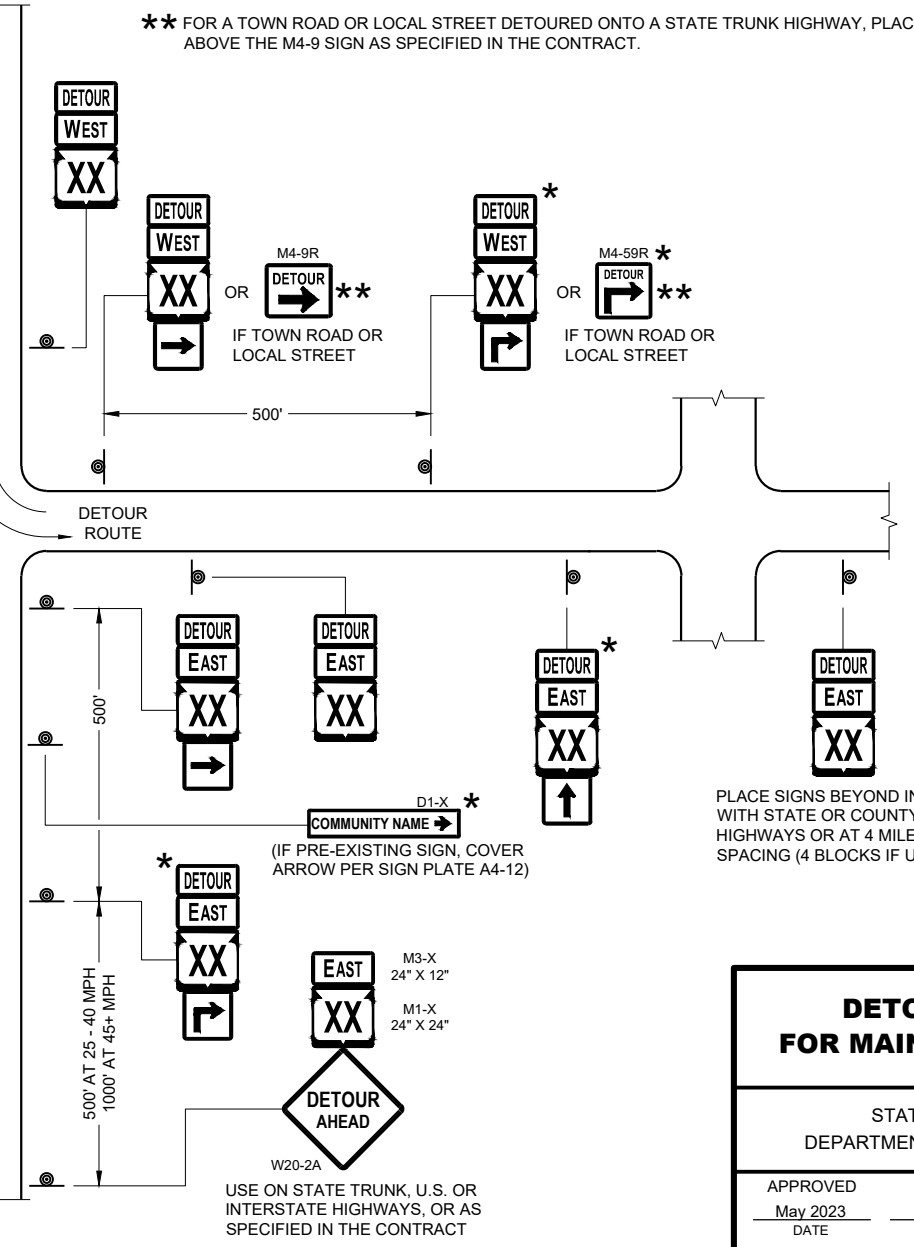
- M3-X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4-8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1-4, M1-5A AND M1-6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- M05-1 AND M06-1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- M4-9 AND M4-59 SHALL BE 30" X 24"
- M4-8a SHALL BE 24" X 18"
- G20-51 SHALL BE 60" X 24"
- W20-2A SHALL BE 48" X 48"
- D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.

- * OPTIONAL SIGNS. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS.
- ** FOR A TOWN ROAD OR LOCAL STREET DETOURED ONTO A STATE TRUNK HIGHWAY, PLACE A ROAD NAME PLAQUE ABOVE THE M4-9 SIGN AS SPECIFIED IN THE CONTRACT.

MATCH POINT

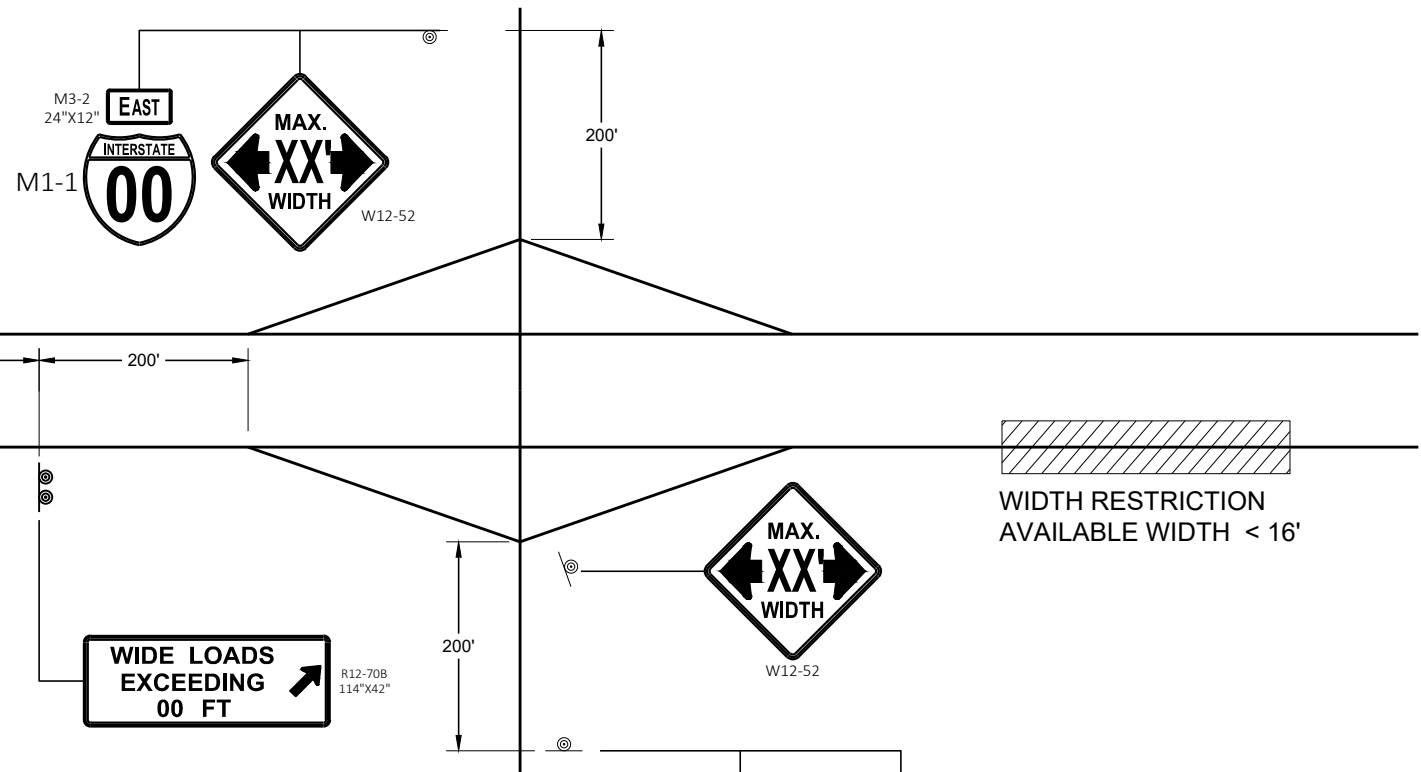


**DETAIL F
DETOUR SIGNING**

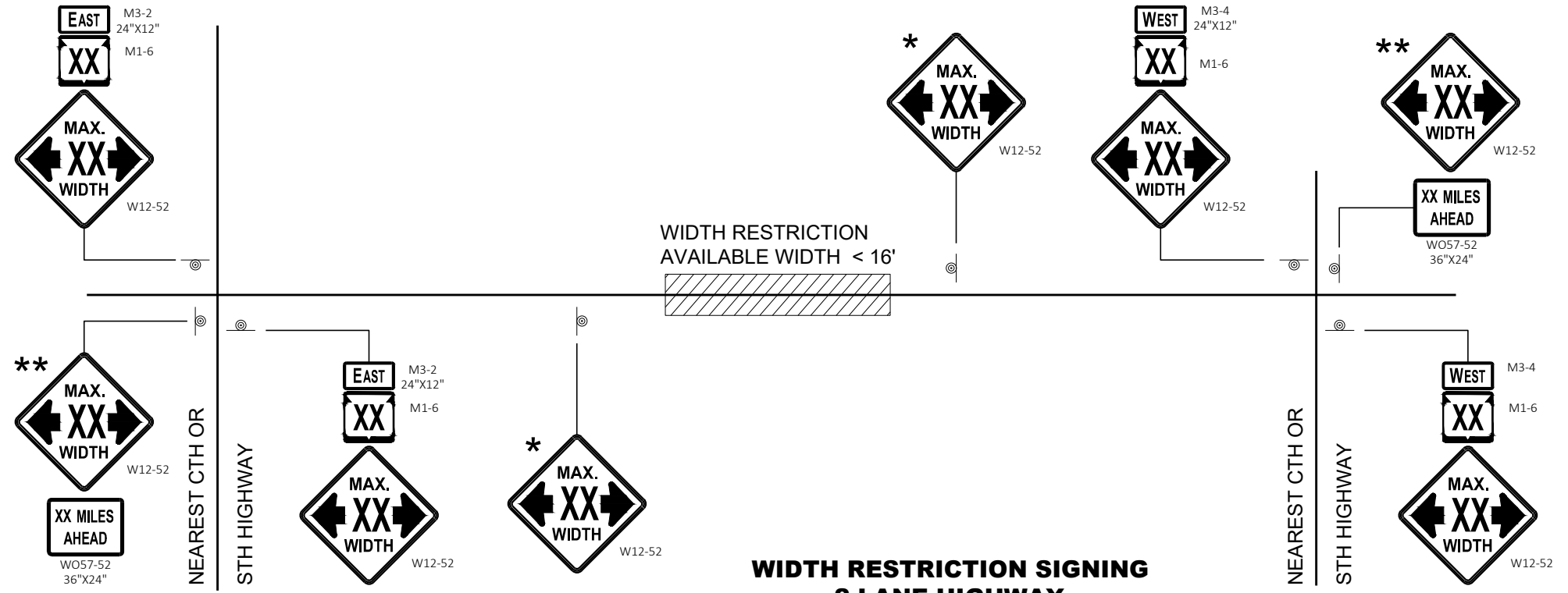


PLACE SIGNS BEYOND INTERSECTIONS WITH STATE OR COUNTY TRUNK HIGHWAYS OR AT 4 MILE MAXIMUM SPACING (4 BLOCKS IF URBAN AREA)

DETOUR SIGNING FOR MAINLINE CLOSURES	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2023 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	



WIDTH RESTRICTION SIGNING



**WIDTH RESTRICTION SIGNING
2 LANE HIGHWAY**

LEGEND

⊙ SIGN ON PERMANENT SUPPORT

GENERAL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

WIDTH ON SIGN TO BE APPROXIMATELY ONE FOOT LESS THAN AVAILABLE WIDTH.

* PLACE 500 FEET AFTER THE W20 - 1A AND 500 FEET BEFORE ADDITIONAL SIGNS FOR ROADWAYS WITH A PRE - CONSTRUCTION SPEED LIMIT OF 45 MPH OR MORE. FOR 35-40 MPH, USE 350 FOOT TYPICAL SPACING. FOR 25-30 MPH, USE 200 FOOT TYPICAL SPACING.

** SIGN SHALL BE VISIBLE FROM ROADWAY.

*** ADDITIONAL SIGNS NEEDED IF THERE IS AN ON RAMP BETWEEN SIGNS.

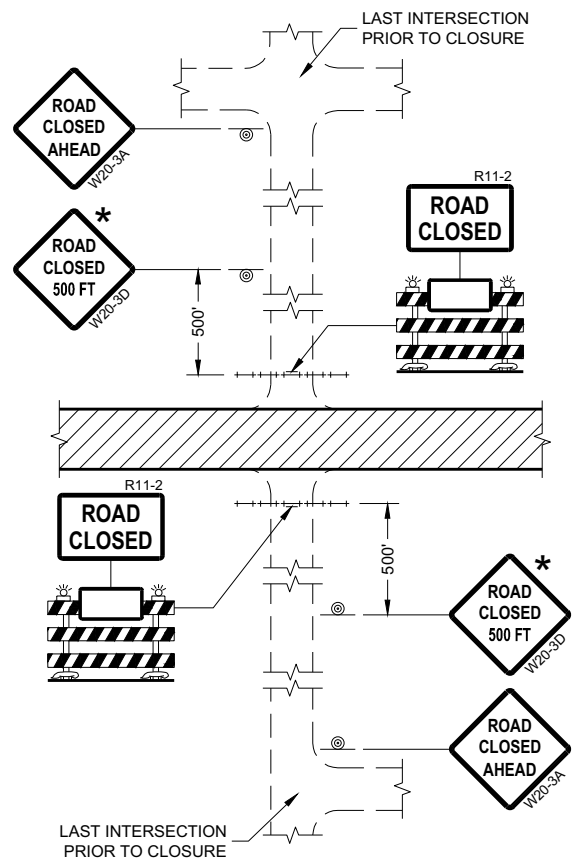


WIDTH ON SIGN TO BE APPROX. 1 - FOOT LESS THAN AVAILABLE WIDTH

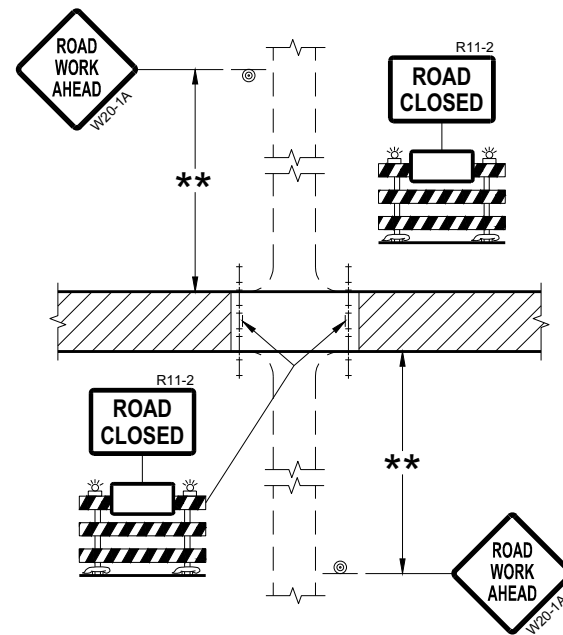
**ADVANCED WIDTH
RESTRICTION SIGNING**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

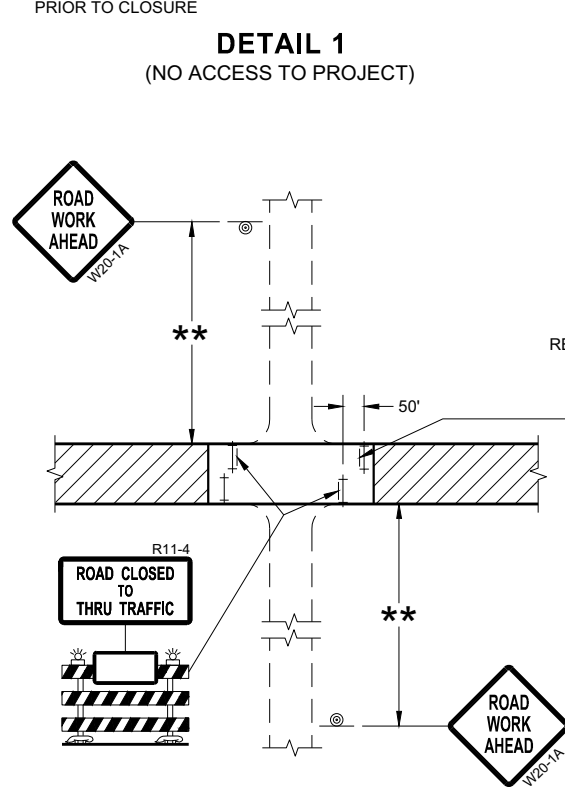
APPROVED	/S/ Andrew Heidtke
DATE	May 2023
FHWA	WORK ZONE ENGINEER



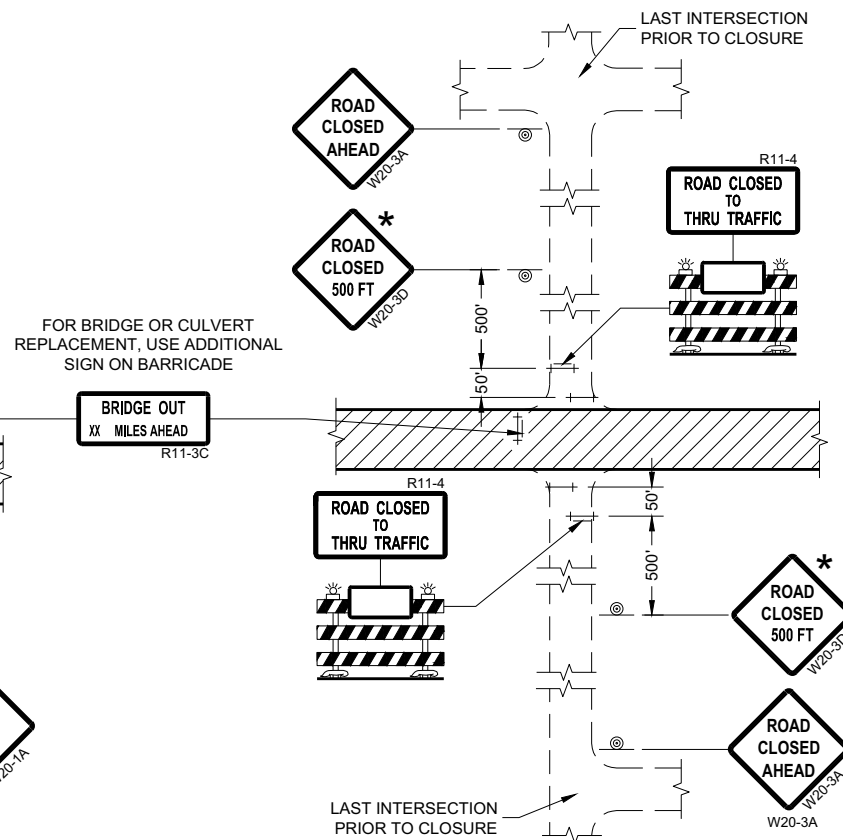
DETAIL 1
(NO ACCESS TO PROJECT)



DETAIL 2
(PUBLIC CROSS-TRAFFIC MAINTAINED.
NO ACCESS TO PROJECT)



DETAIL 3
(PUBLIC CROSS-TRAFFIC MAINTAINED.
CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)



DETAIL 4
(CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE (500 FEET DESIRABLE) TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3, AND R11-4 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
R11-2 SHALL BE 48" X 30".
R11-4 AND R11-3 SHALL BE 60" X 30".

- * OMIT THE "ROAD CLOSED 500 FT." SIGN IF THE LAST INTERSECTION IS 500 FEET OR LESS FROM THE WORK ZONE.
- ** 500' MAX. OR AT LAST INTERSECTION, WHICHEVER IS CLOSEST.

LEGEND

- ⊙ SIGN ON PERMANENT SUPPORT
- ⊥ TYPE III BARRICADE
- ⊥ TYPE III BARRICADE WITH ATTACHED SIGN
- ⚡ TYPE "A" WARNING LIGHT (FLASHING)
- ▨ WORK AREA

**BARRICADES AND SIGNS
FOR
SIDEROAD CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
July 2018 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.




ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

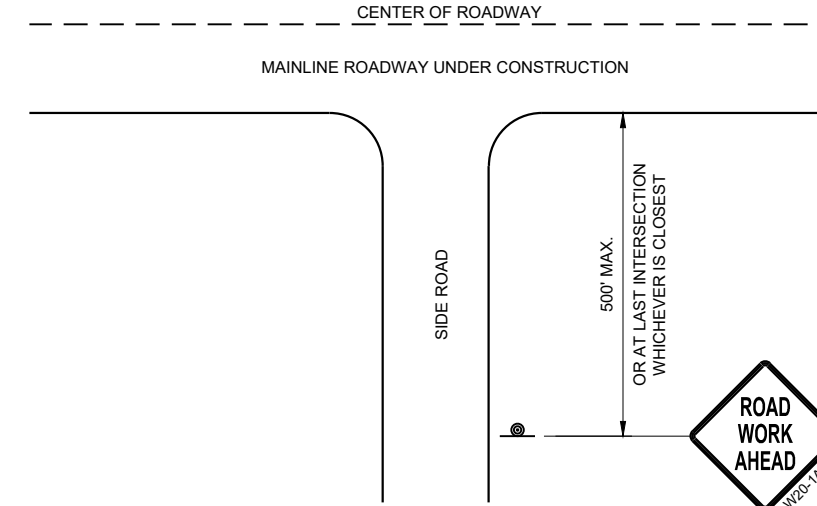
SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

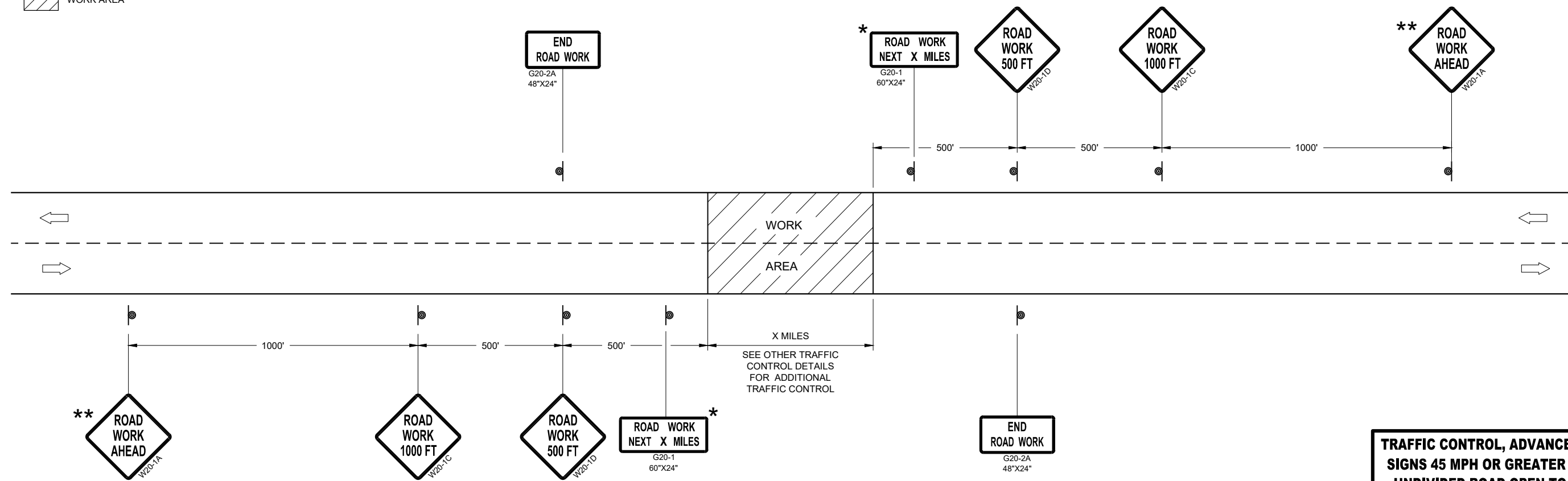
- * OMIT G20-1 SIGNS IF LENGTH OF WORK AREA IS 2 MILES OR LESS
- ** PLACE AN ADDITIONAL W20-1A "ROAD WORK AHEAD" SIGN IF WORK AREA WITHIN THE PROJECT IS SEPARATED BY MORE THAN 2 MILES FROM PREVIOUS WORK AREA.

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC
-  WORK AREA

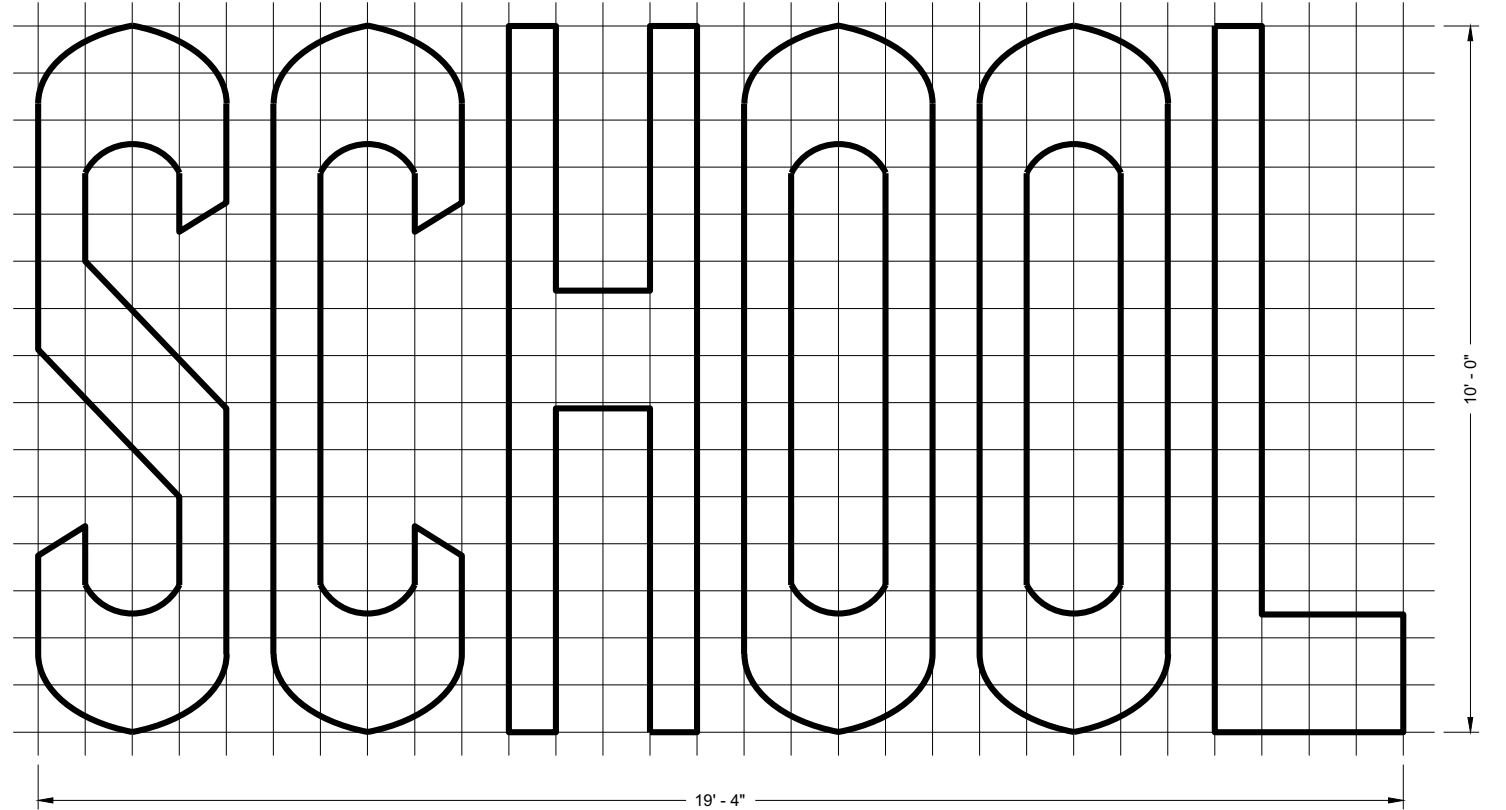
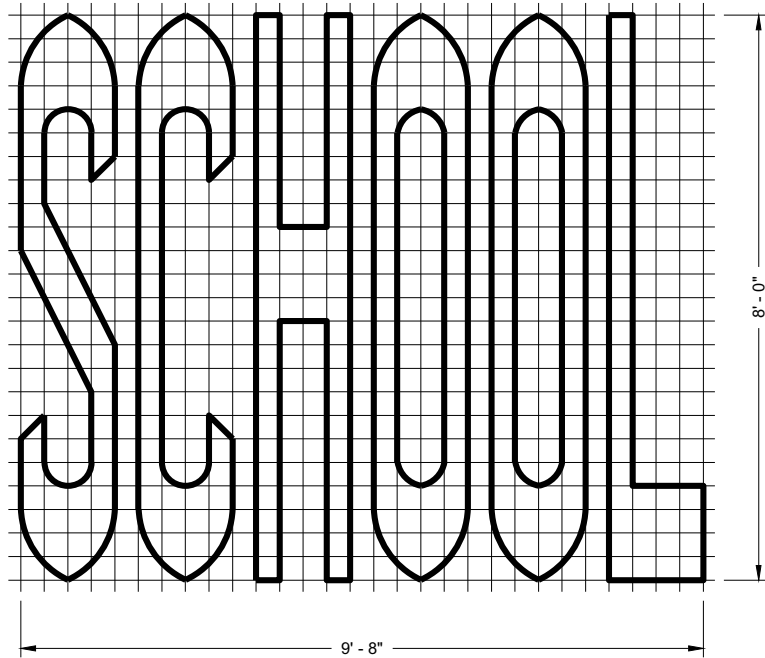
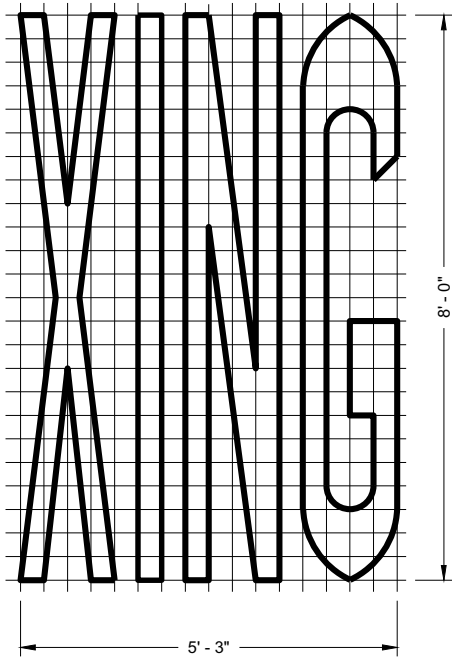
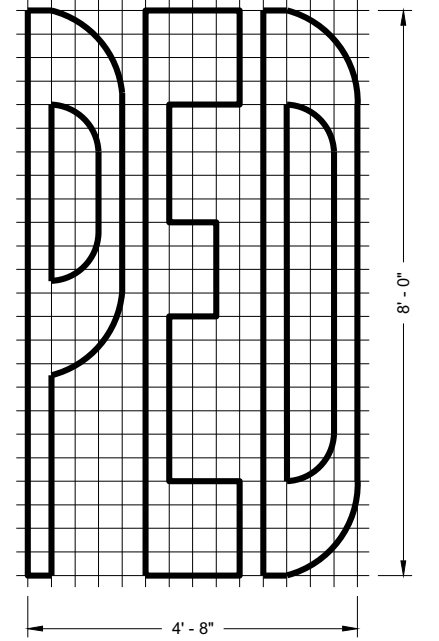
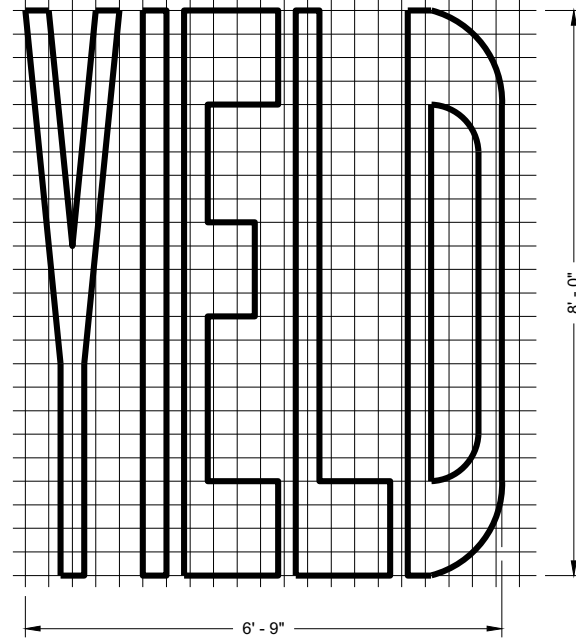
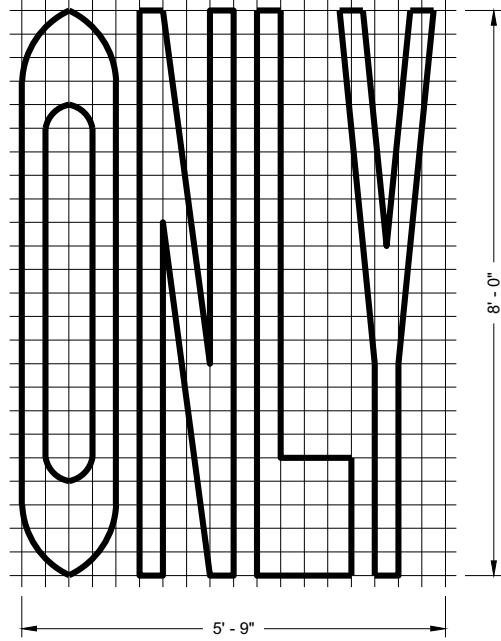
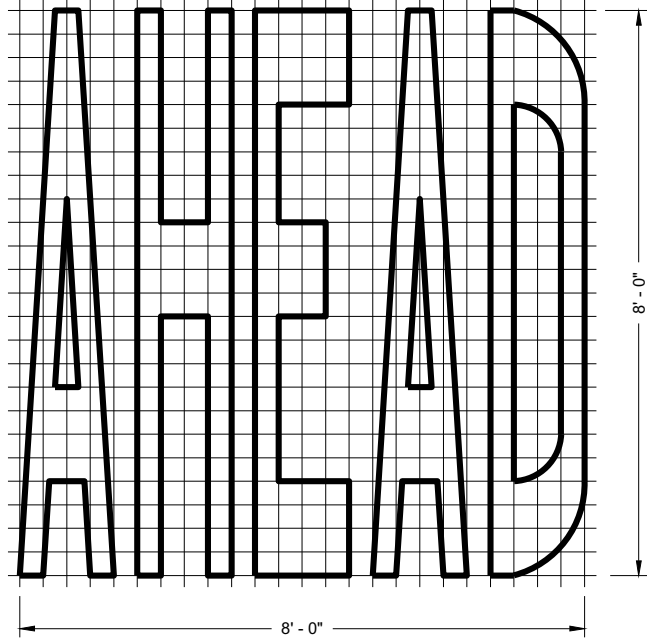
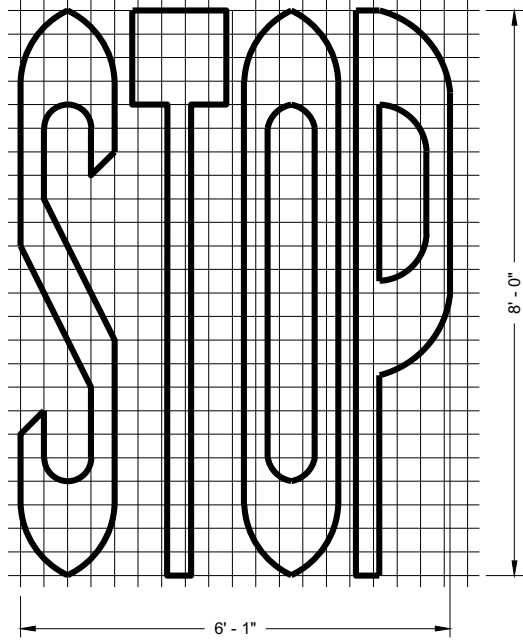


TYPICAL SIDE ROAD APPROACH WARNING SIGN DETAIL



TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45MPH OR GREATER

TRAFFIC CONTROL, ADVANCE WARNING SIGNS 45 MPH OR GREATER TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	



SINGLE LANE

TWO - LANE

GENERAL NOTES

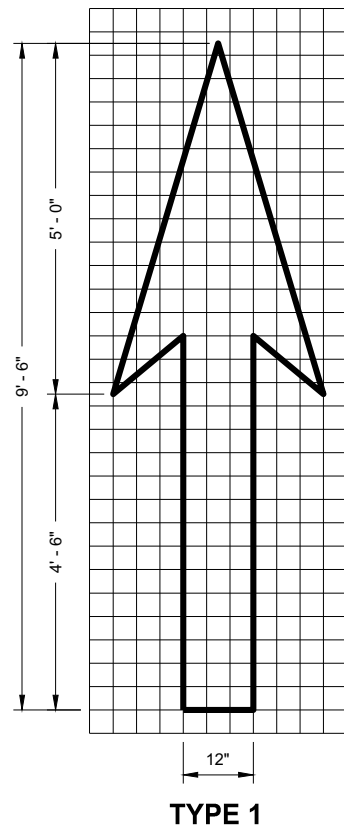
DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

PAVEMENT MARKING WORDS

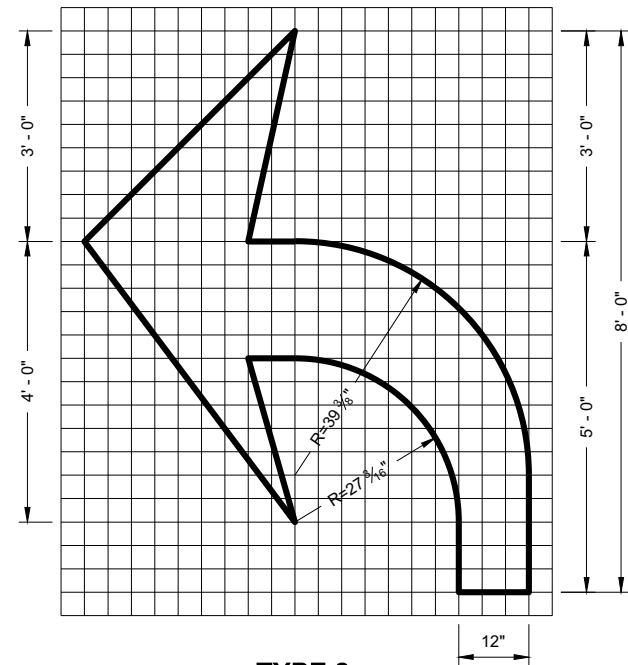
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2019 /S/ Matthew Rauch
DATE STATE SIGNING AND MARKING
ENGINEER

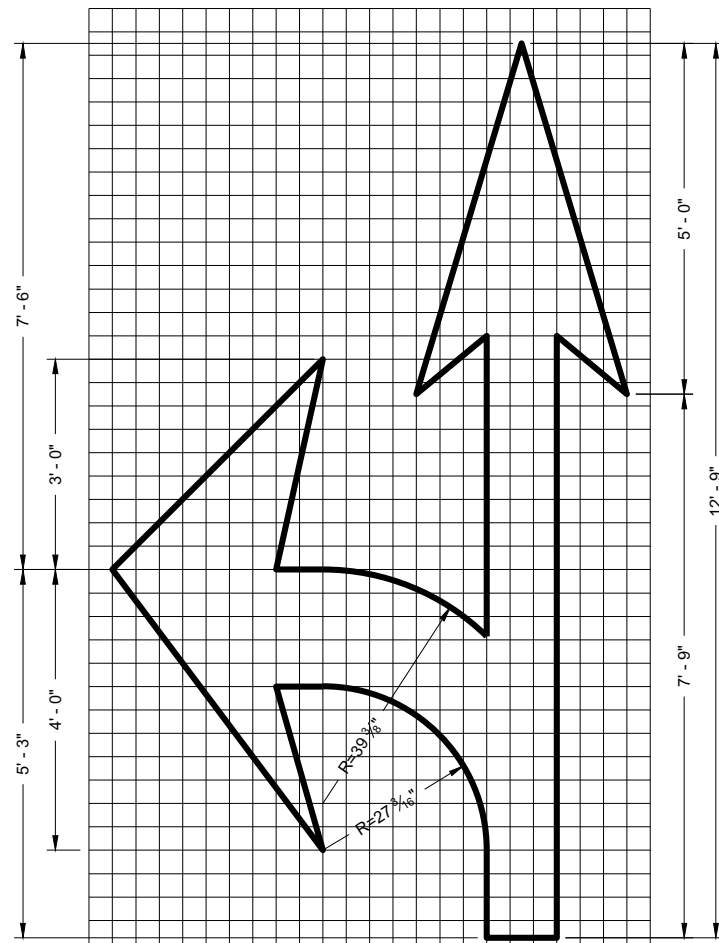
FHWA



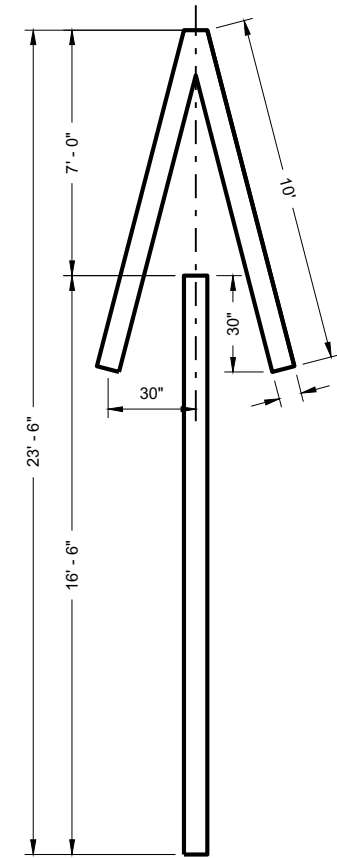
TYPE 1



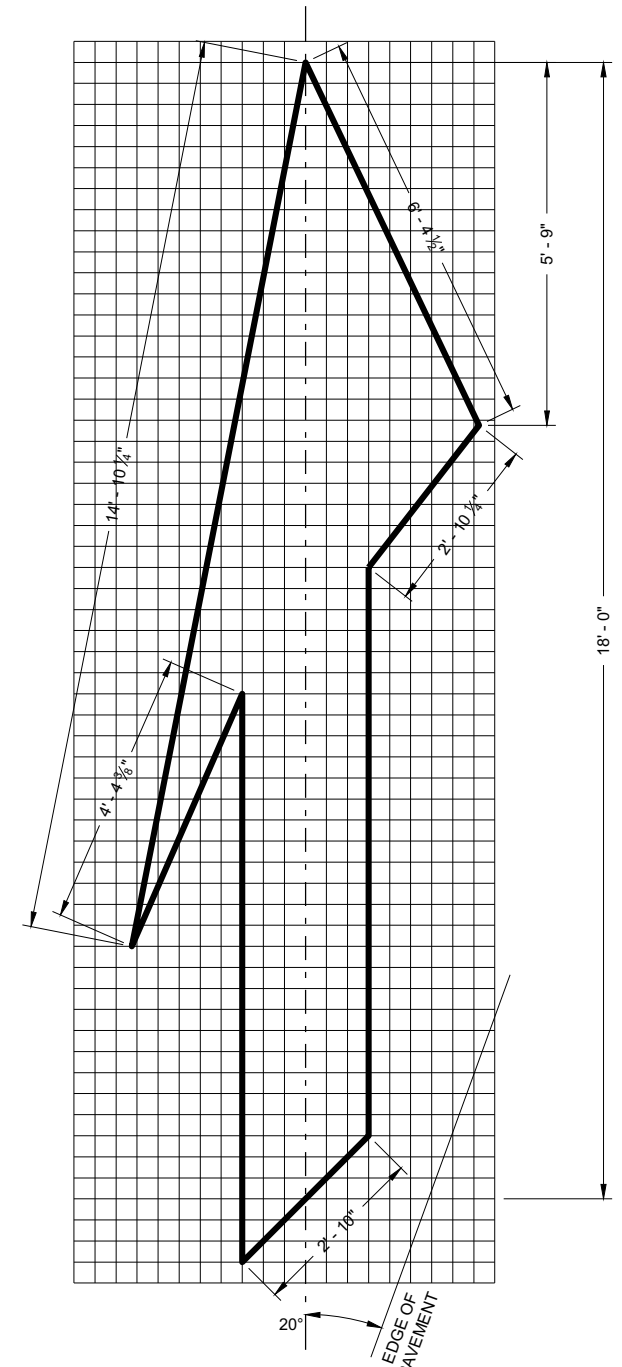
TYPE 2



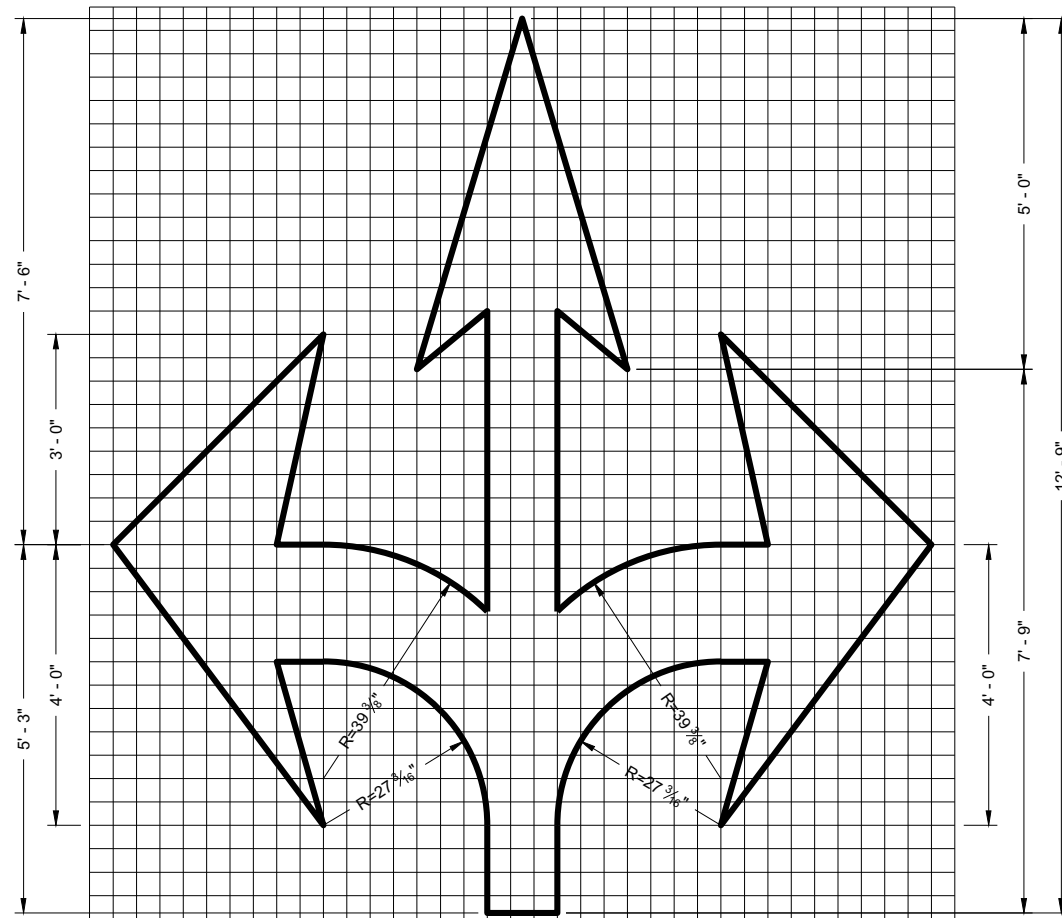
TYPE 3



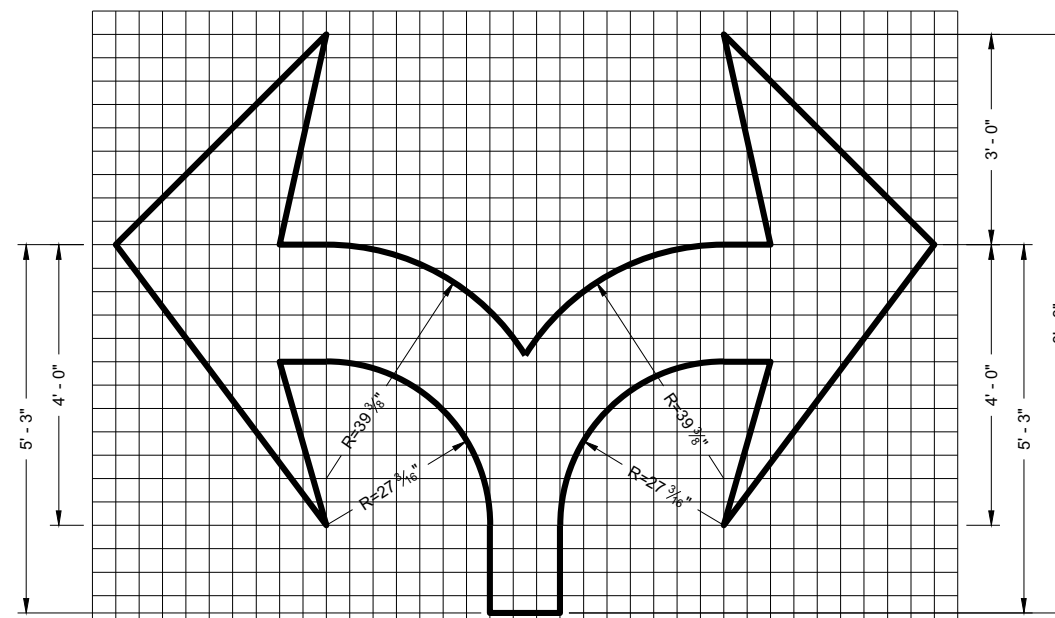
TYPE 4



TYPE 5 LANE DROP ARROW



TYPE 6



TYPE 7

GENERAL NOTES

DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

PAVEMENT MARKING ARROWS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2019
DATE

/S/ Matthew Rauch
STATE SIGNING AND MARKING
ENGINEER




FHWA

GENERAL NOTES

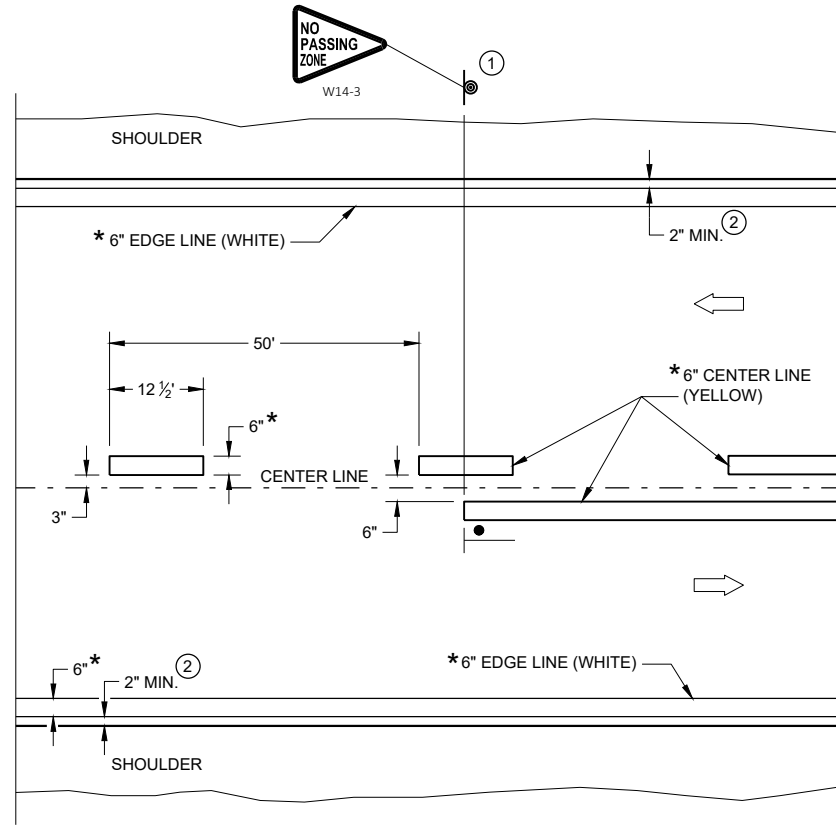
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

- ① LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING
- ② MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

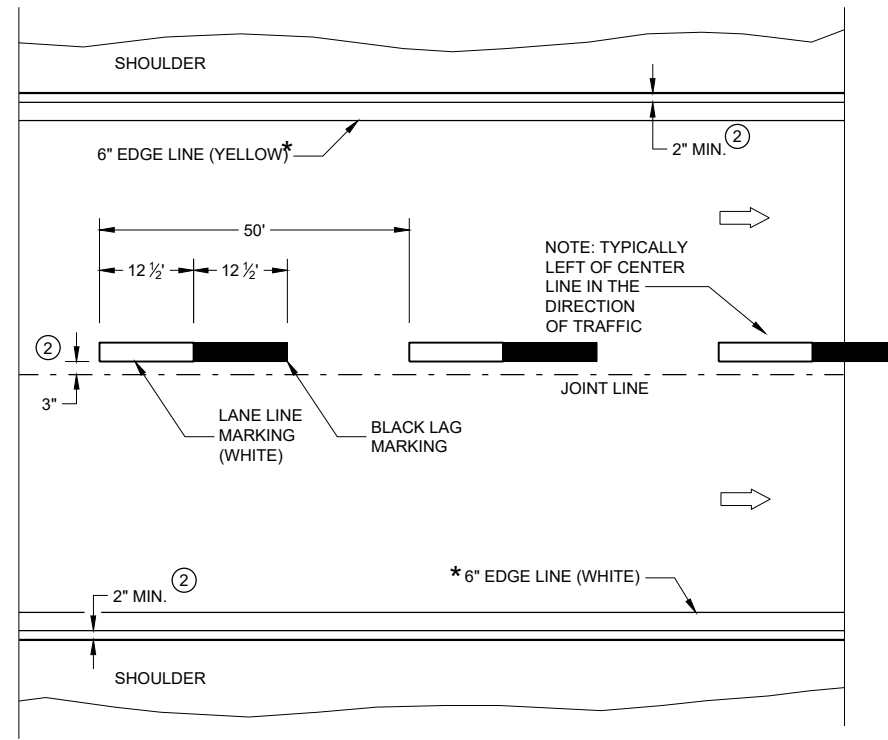
LEGEND

-  "T" MARKING
-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC

* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES



TWO WAY TRAFFIC



ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING

6

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SDD 15C08-23a

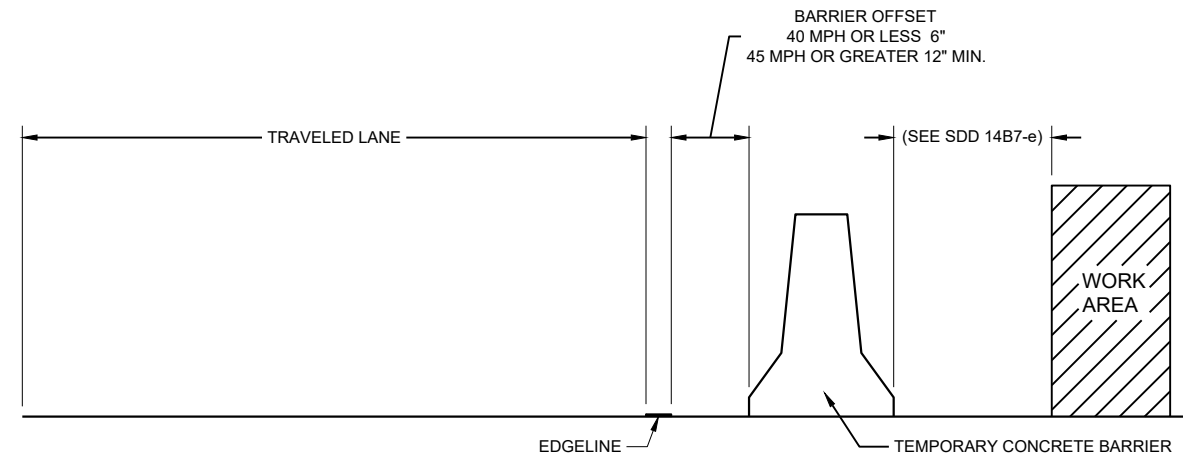
SDD 15C08-23a

PERMANENT LONGITUDINAL PAVEMENT MARKINGS

STATE OF WISCONSIN
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APPROVED
May 2023 /S/ Jeannie Silver
DATE STATEWIDE SIGNING AND MARKING ENGINEER

FHWA



TEMPORARY BARRIER OFFSET FROM EDGELINE

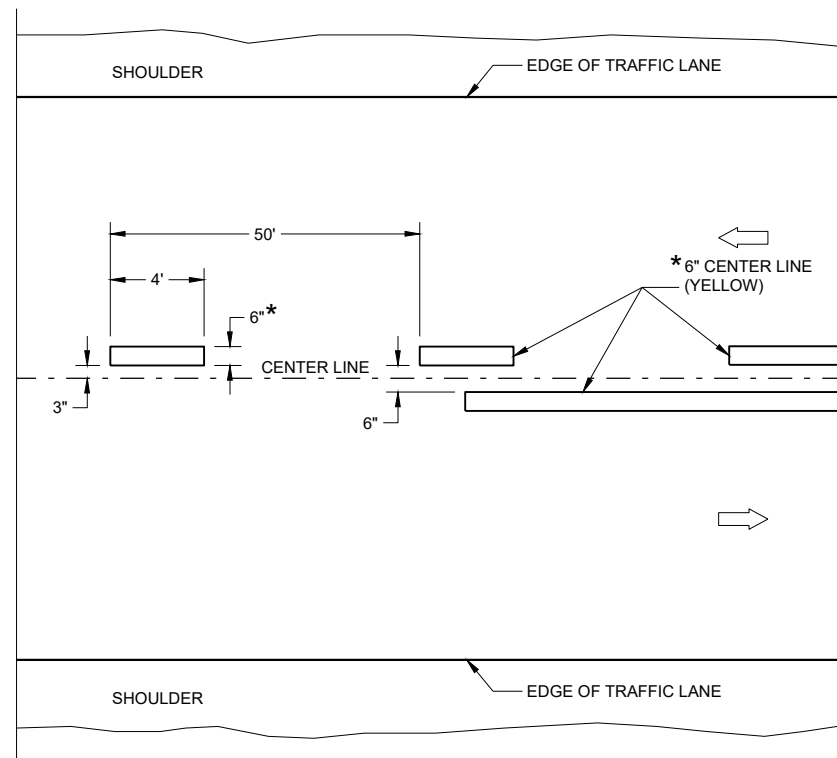
GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

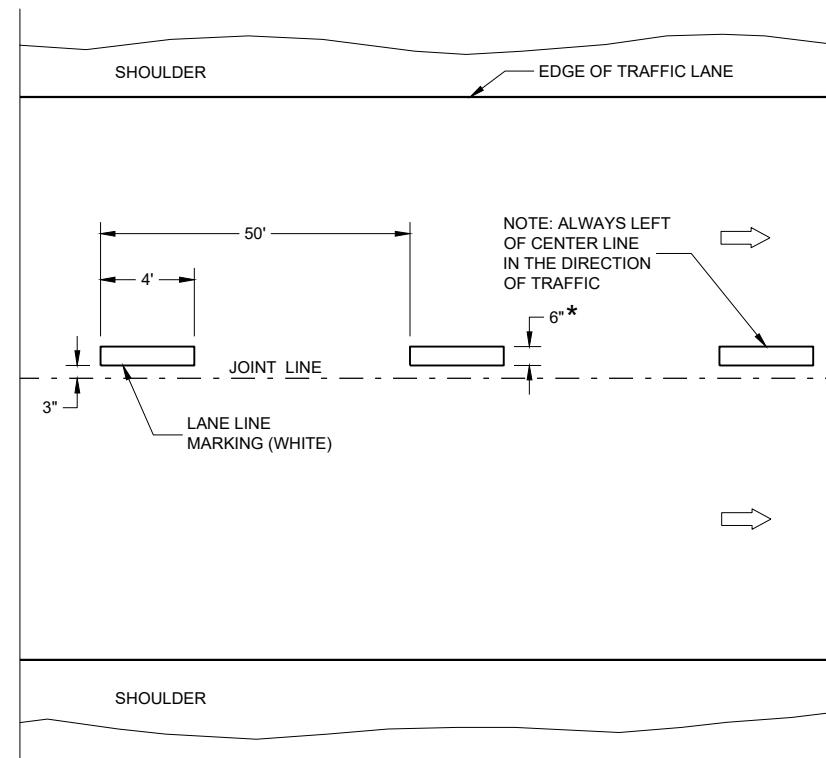
LEGEND

➡ DIRECTION OF TRAFFIC

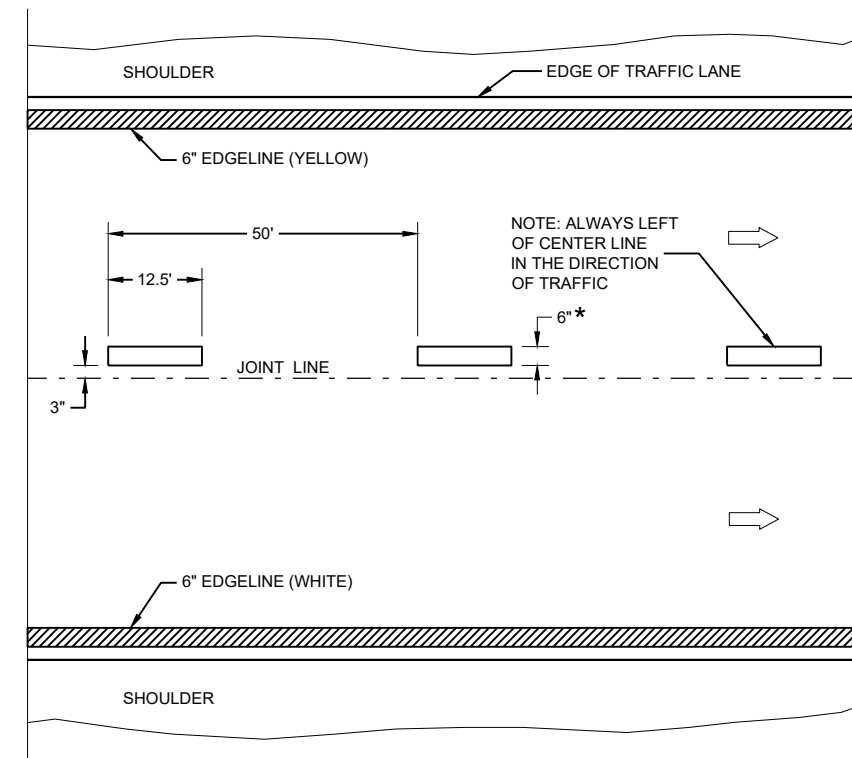
* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES



TWO WAY TRAFFIC



ONE WAY TRAFFIC



FREEWAYS AND EXPRESSWAYS

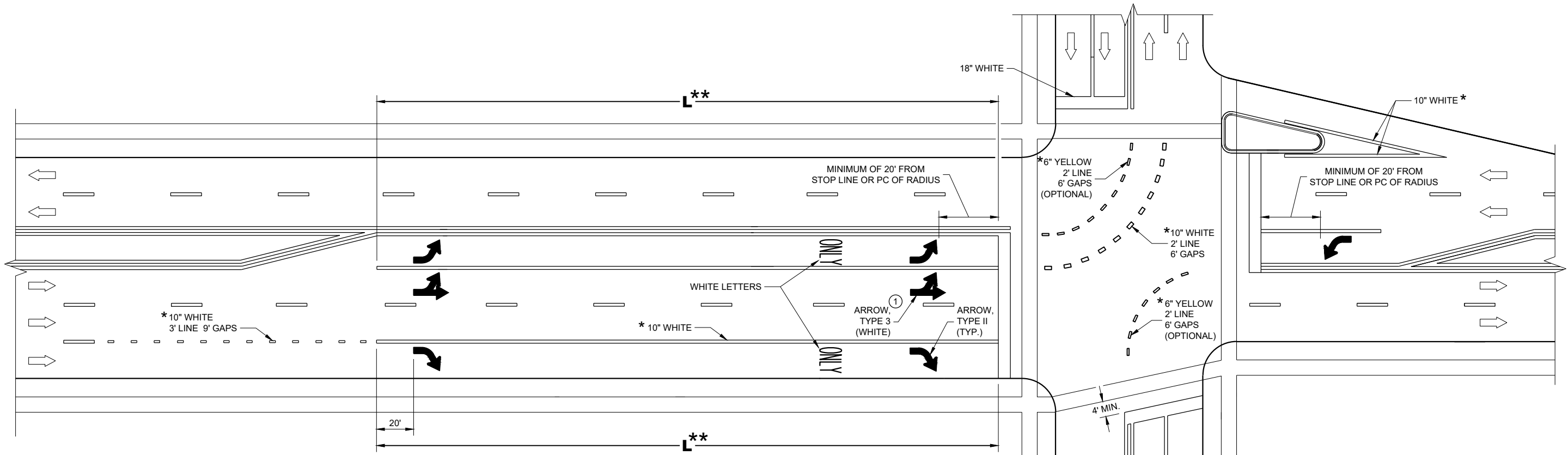
TEMPORARY PAVEMENT MARKING

TEMPORARY LONGITUDINAL PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

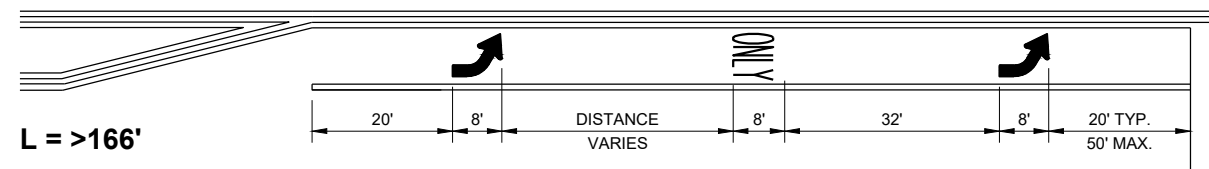
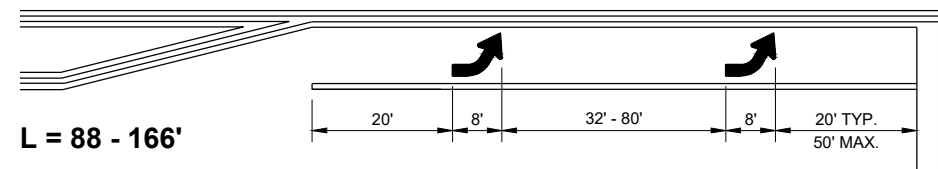
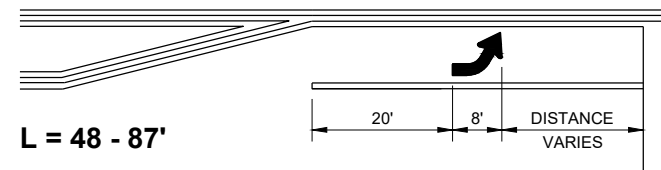
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May 2023 /S/ Jeannie Silver
DATE STATEWIDE SIGNING AND MARKING ENGINEER

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TURN LANE OPTIONS

LENGTH OF TURN BAY (**L**) OF 0 - 47' DOES NOT REQUIRE PAVEMENT MARKING ARROWS OR WORDS



** (SEE TURN LANE OPTIONS FOR PLACEMENT OF PAVEMENT MARKING ARROWS AND WORDS)

GENERAL NOTES

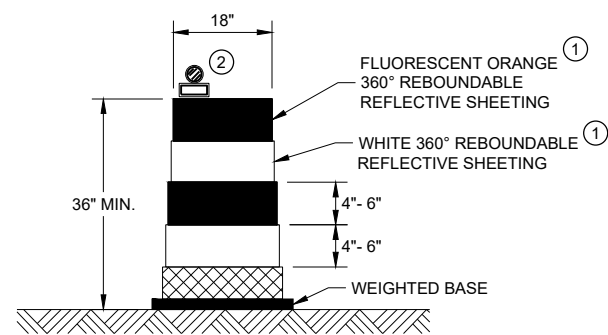
① QUANTITY AND LOCATION OF TYPE 3 ARROWS ARE THE SAME AS THE TYPE II ARROWS IN THE ADJACENT TURN LANE. FOR TURN LANES WITH A PHYSICAL SEPARATION IN THE SAME DIRECTION OF TRAVEL, THE ARROWS AND "ONLY" MARKING MAY BE ELIMINATED.

➡ DIRECTION OF TRAFFIC

L = LENGTH OF TURN BAY

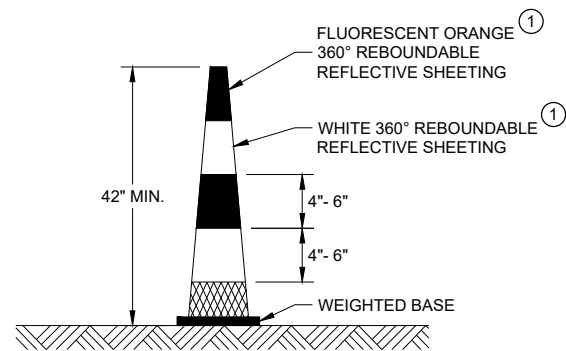
* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES

PAVEMENT MARKING (TURN LANES)
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION



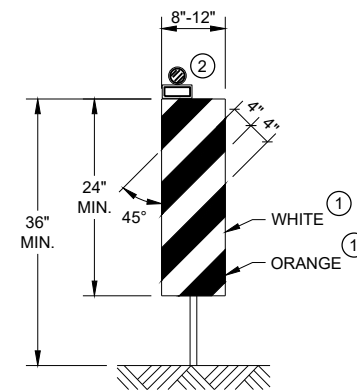
DRUM

BALLAST WIDTHS
RANGE FROM 24"-36"



42" CONE

DO NOT USE IN TAPERS
½ SPACING OF DRUMS
BALLAST WIDTHS
RANGE FROM 14"-20"

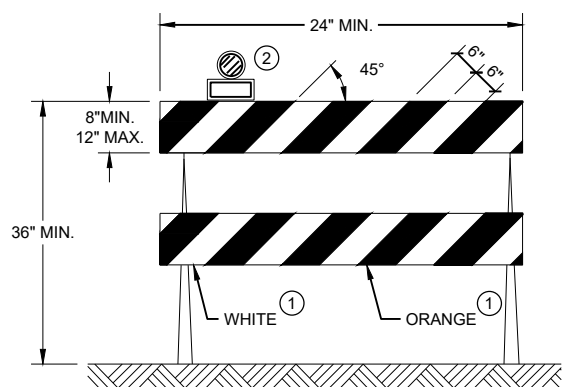


VERTICAL PANEL

THE STRIPES SHALL SLOPE DOWNWARD TO
THE TRAFFIC SIDE FOR CHANNELIZATION.

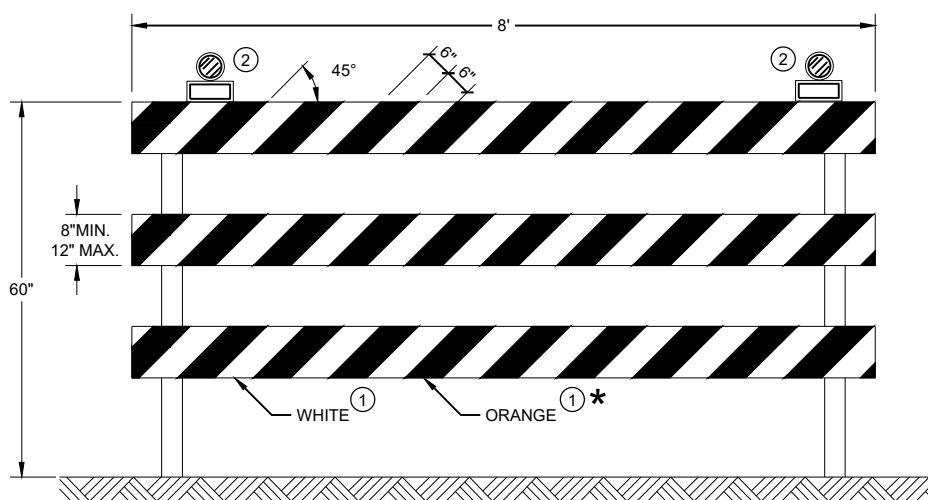
GENERAL NOTES

- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.



TYPE II BARRICADE

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES
MAY BE USED. ALL STRIPES SHALL SLOPE DOWNWARD
TO THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE III BARRICADE

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP
TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.

* IF USED FOR A PERMANENT APPLICATION USE RED SHEETING.

**CHANNELIZING DEVICES
DRUMS, CONES, BARRICADES
AND VERTICAL PANELS**

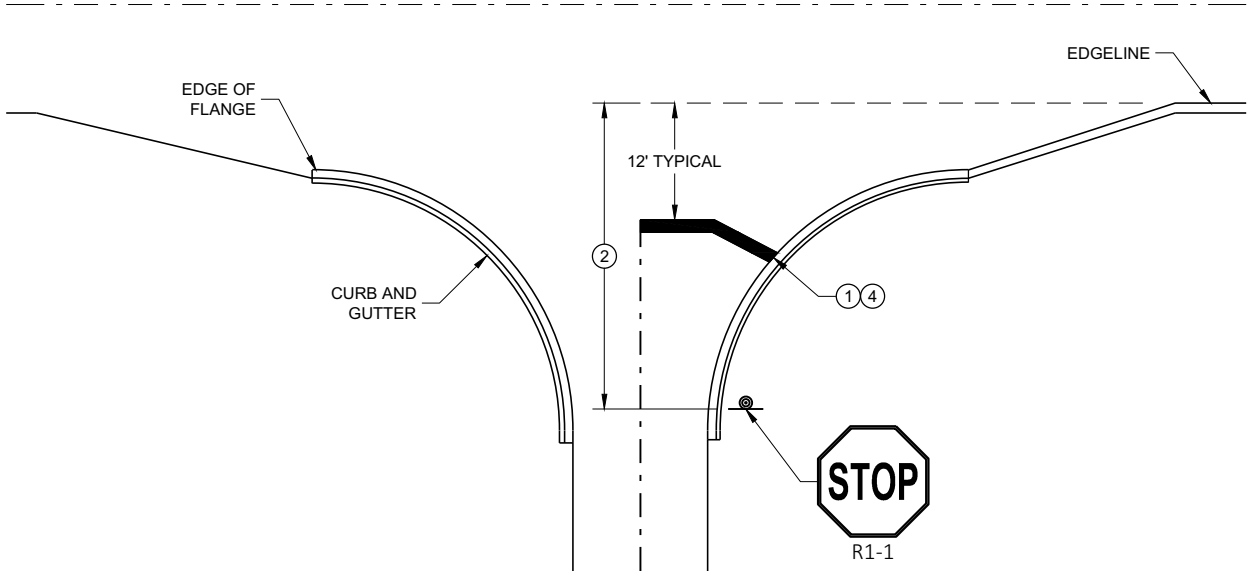
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2022 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER
FHWA

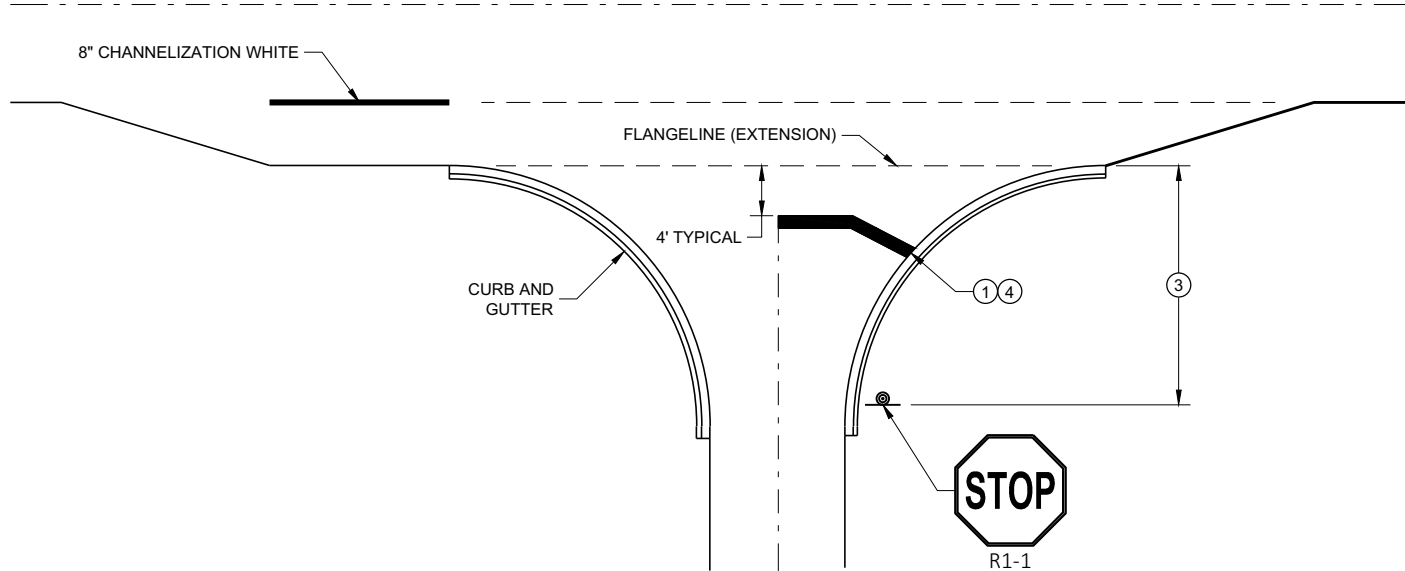
GENERAL NOTES

STOP SIGN SHALL BE PLACED A MINIMUM OF 6 FEET TO A MAXIMUM OF 50 FEET FROM THE EDGELINE LOCATION.

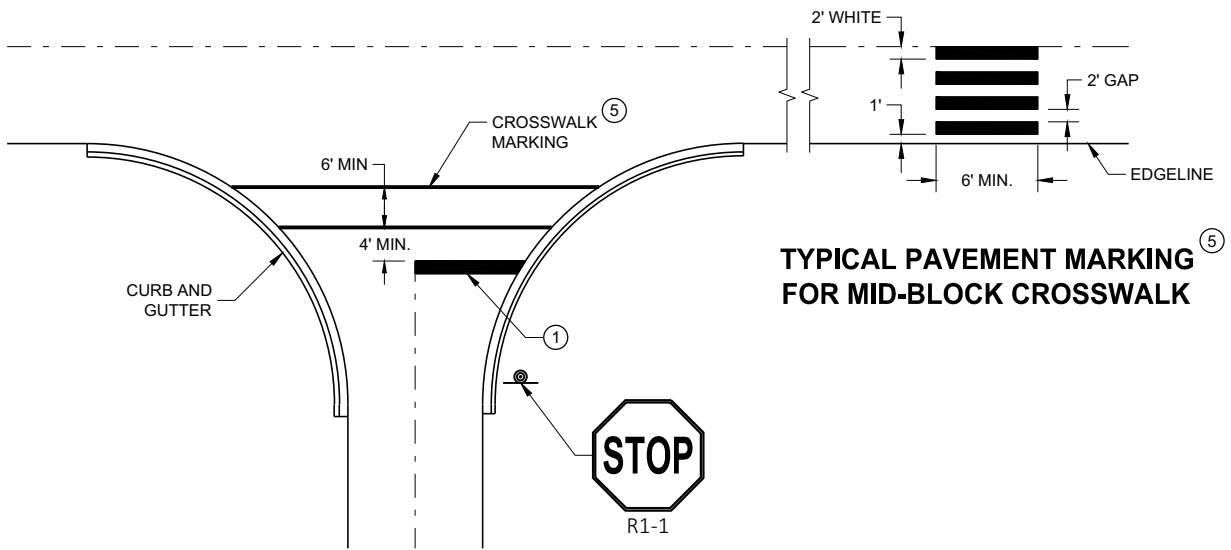
- ① 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE REGION MARKING ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
- ② NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGELINE.
- ③ NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGELINE EXTENSION.
- ④ MOVE CLOSER TO THE EDGE OF TRAVEL LINE AS NEEDED FOR VISIBILITY AND SIGHT LINES (NO CLOSER THAN 4 FEET).
- ⑤ LADDER BAR CROSSWALKS SHOULD ONLY BE USED FOR MID BLOCK CROSSINGS. USE 2 - 6" TRANSVERSE LINES INSTEAD.



TYPICAL STOP LINE PAVEMENT MARKING WITH CURB AND GUTTER

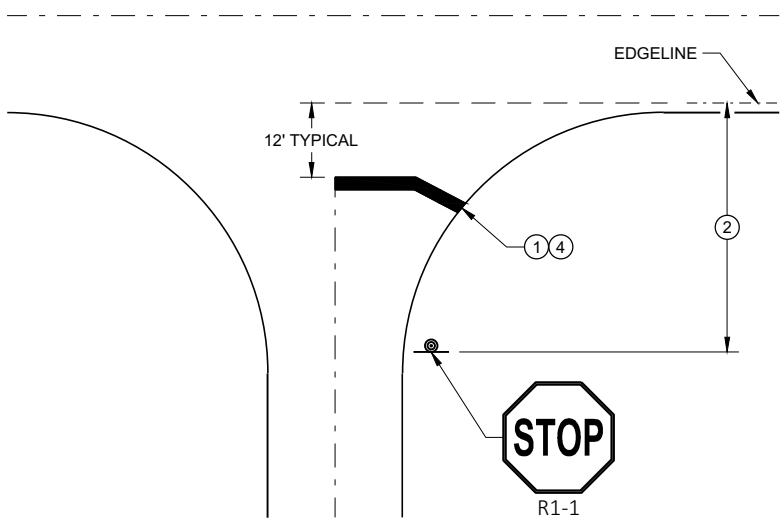


TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH RIGHT TURN LANE



TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH CROSSWALK MARKING

TYPICAL PAVEMENT MARKING FOR MID-BLOCK CROSSWALK



TYPICAL STOP LINE PAVEMENT MARKING WITHOUT CURB AND GUTTER

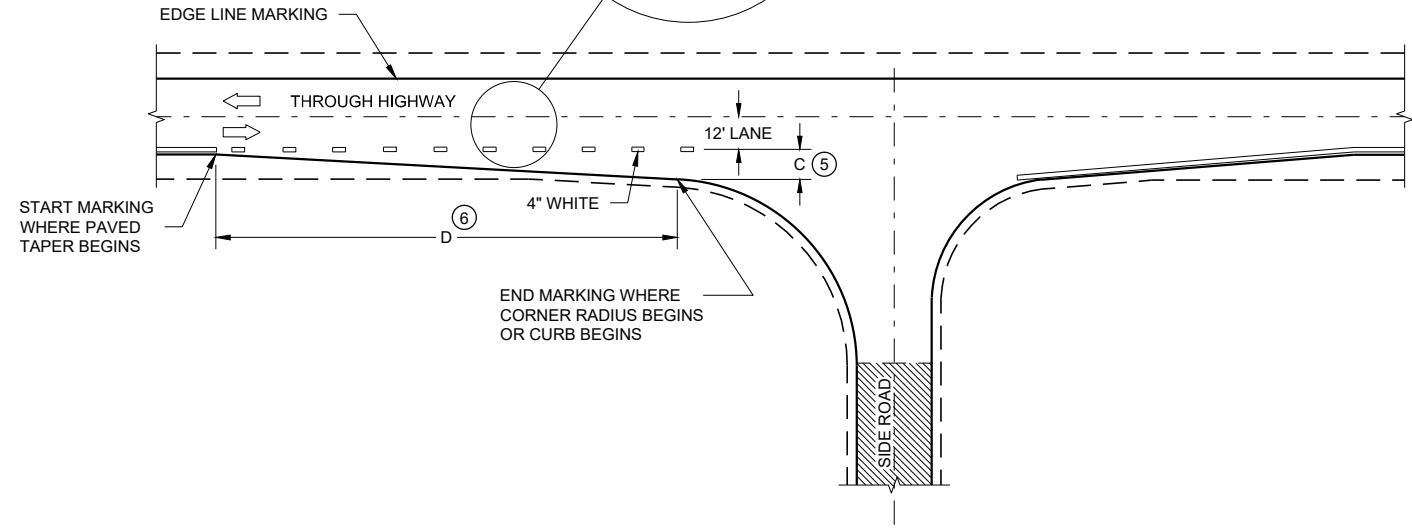
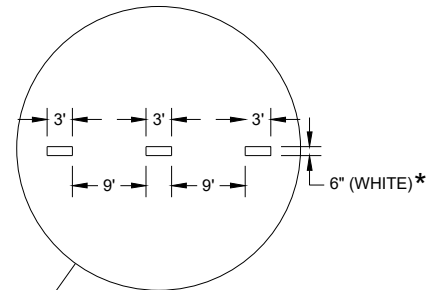
STOP LINE AND CROSSWALK PAVEMENT MARKING	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED November 2019 DATE	/s/ Matthew Rauch STATE SIGNING AND MARKING ENGINEER
FHWA	

6

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SDD 15C33 - 04

SDD 15C33 - 04



MINOR INTERSECTION

* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES

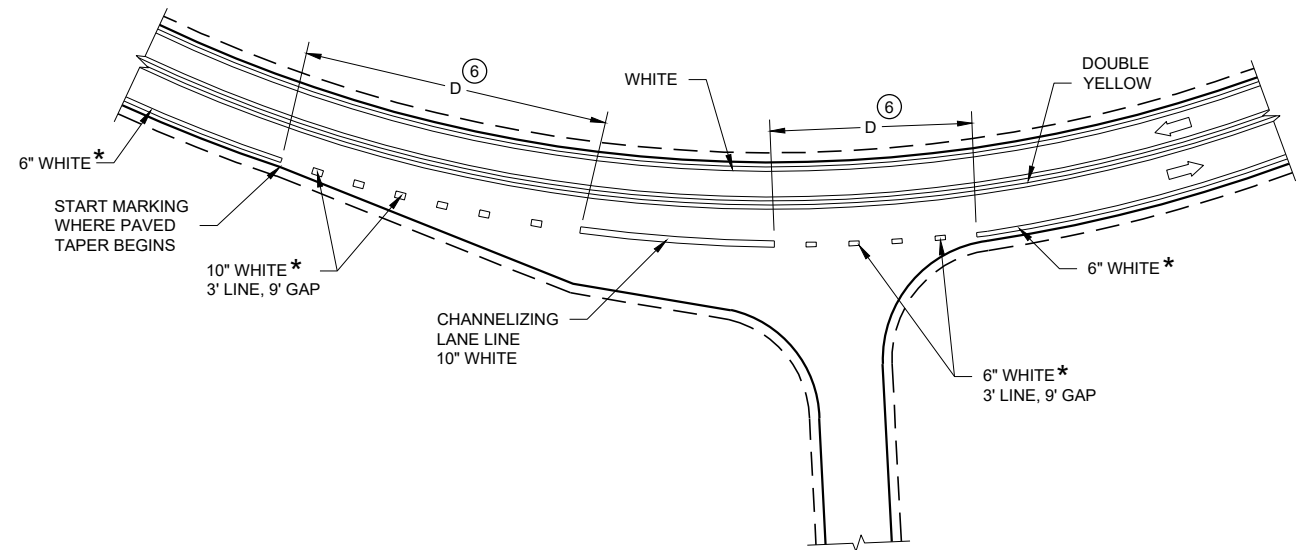
GENERAL NOTES

OMIT EDGE LINES THROUGH INTERSECTIONS. CONTINUE EDGE LINES THROUGH DRIVEWAYS.

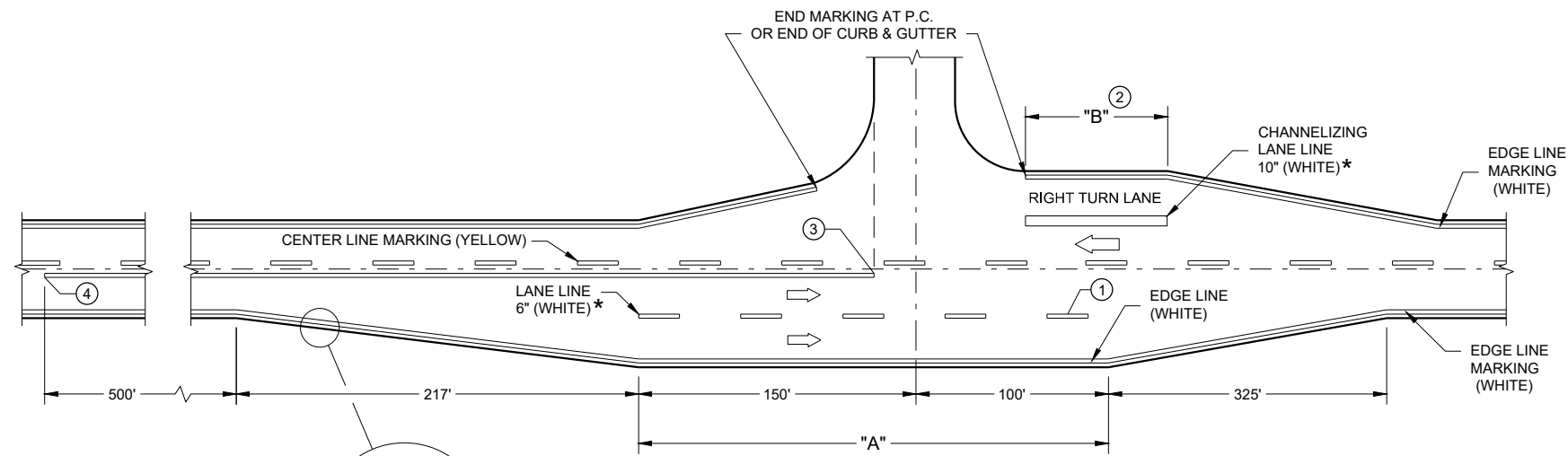
- ① WHEN DISTANCE "A" IS LESS THAN 250 FEET, OMIT LANE LINE.
- ② WHEN DISTANCE "B" IS LESS THAN 100 FEET, OMIT CHANNELIZING LANE LINE.
- ③ BARRIER LINE ENDS AT SIDE ROAD PAVEMENT / SURFACE EDGE EXTENSION.
- ④ BARRIER LINE STARTS 500 FEET PRIOR TO THE BYPASS TAPER.
- ⑤ WHEN DISTANCE "C" IS LESS THAN 4 FEET, OMIT DOTTED EXTENSION.
- ⑥ WHEN DISTANCE "D" IS LESS THAN 50 FEET, OMIT DOTTED EXTENSION.

LEGEND

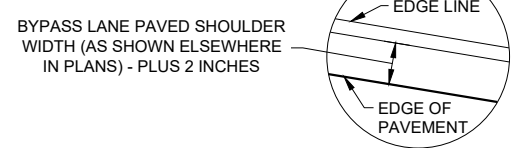
➔ DIRECTION OF TRAVEL



INTERSECTION ON OUTSIDE OF CURVE



**MAJOR INTERSECTIONS
(INTERSECTION WITH FULL RIGHT TURN LANE OR BYPASS LANE)**







BYPASS LANE PAVED SHOULDER WIDTH (AS SHOWN ELSEWHERE IN PLANS) - PLUS 2 INCHES

**PAVEMENT MARKING
(INTERSECTIONS)**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  DIRECTION OF TRAFFIC
-  WORK ZONE

GENERAL NOTES

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS IN URBAN AREAS, 36" X 36" SIGNS MAY BE USED IF APPROVED BY THE REGIONAL TRAFFIC UNIT.

"WO" SIGN IS THE SAME AS "W" SIGN EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

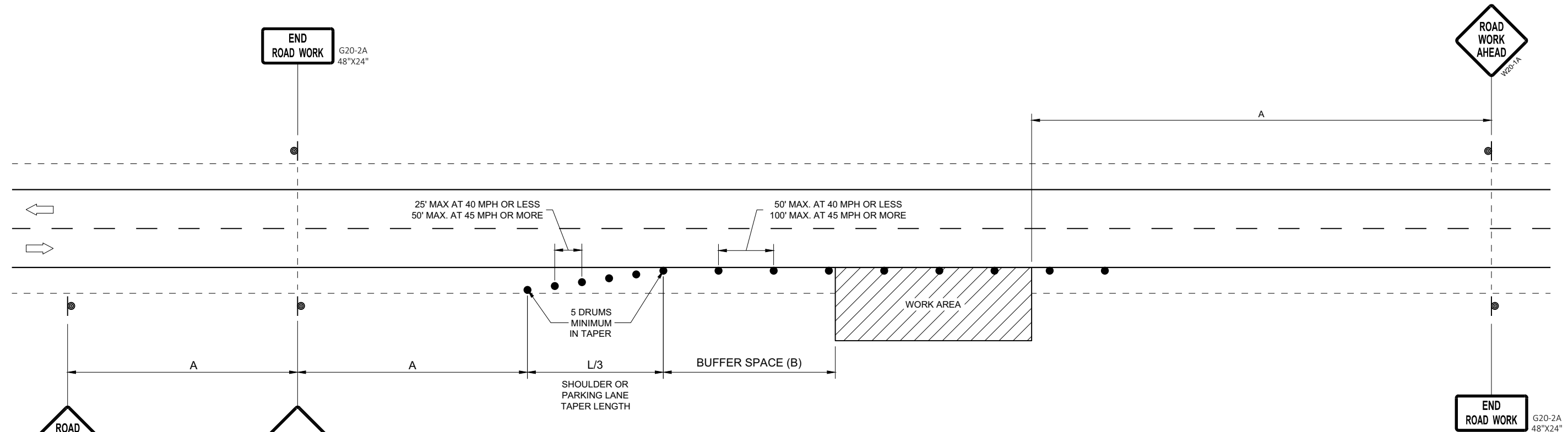
CHANNELIZING DEVICES PLACED ADJACENT TO WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

W20-1A AND G20-2A SIGNS ARE NOT REQUIRED IF THE WORK AREA IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT. G20-2A SIGNS MAY ALSO BE OMITTED IF DURATION OF WORK IS LESS THAN 7 CONTINUOUS DAYS AND NIGHTS.

6

6



POSTED SPEED LIMIT PRIOR TO WORK STARTING (MPH)	ADVANCE WARNING SIGN SPACING (A) FEET	SHOULDER TAPER L / 3 W, LATERAL OFFSET (FT)						BUFFER SPACE (B) FEET
		3	4	5	6	7	8	
25	200'	10	14	17	21	24	28	55
30	200'	15	20	25	30	35	40	85
35	350'	20	27	34	40	47	54	120
40	350'	26	35	44	53	62	70	170
45	500'	45	59	74	89	104	119	220
50	500'	50	66	83	99	116	132	280
55	500'	54	73	91	109	127	145	335'

**TRAFFIC CONTROL, WORK ON
SHOULDER OR PARKING LANE,
UNDIVIDED ROADWAY**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2020 /S/ Andrew Heidtke
DATE STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER

FHWA

SDD 15D28 - 04

SDD 15D28 - 04

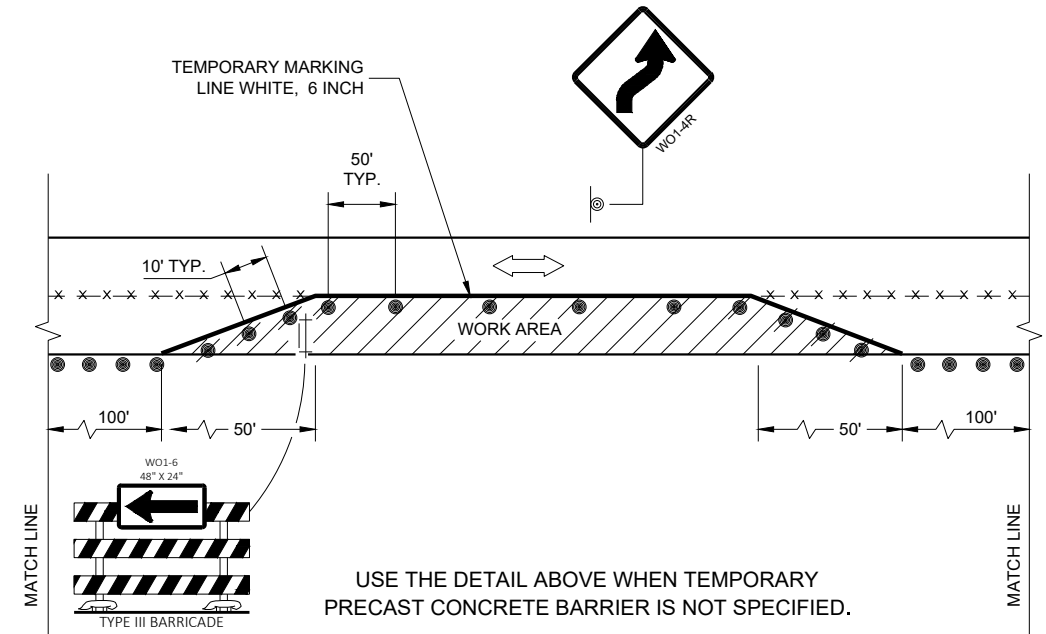
LEGEND

- TYPE III BARRICADE WITH ATTACHED SIGN
- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- TRAFFIC CONTROL DRUM
- FLAGS, 16" X 16" MIN. (ORANGE)
- REMOVING PAVEMENT MARKING
- DIRECTION OF TRAFFIC
- ASPHALTIC PAVEMENT WIDENING
- CONCRETE BARRIER TEMPORARY PRECAST
- TEMPORARY SIGNAL. SEE SDD 09G02 FOR EXACT PLACEMENT

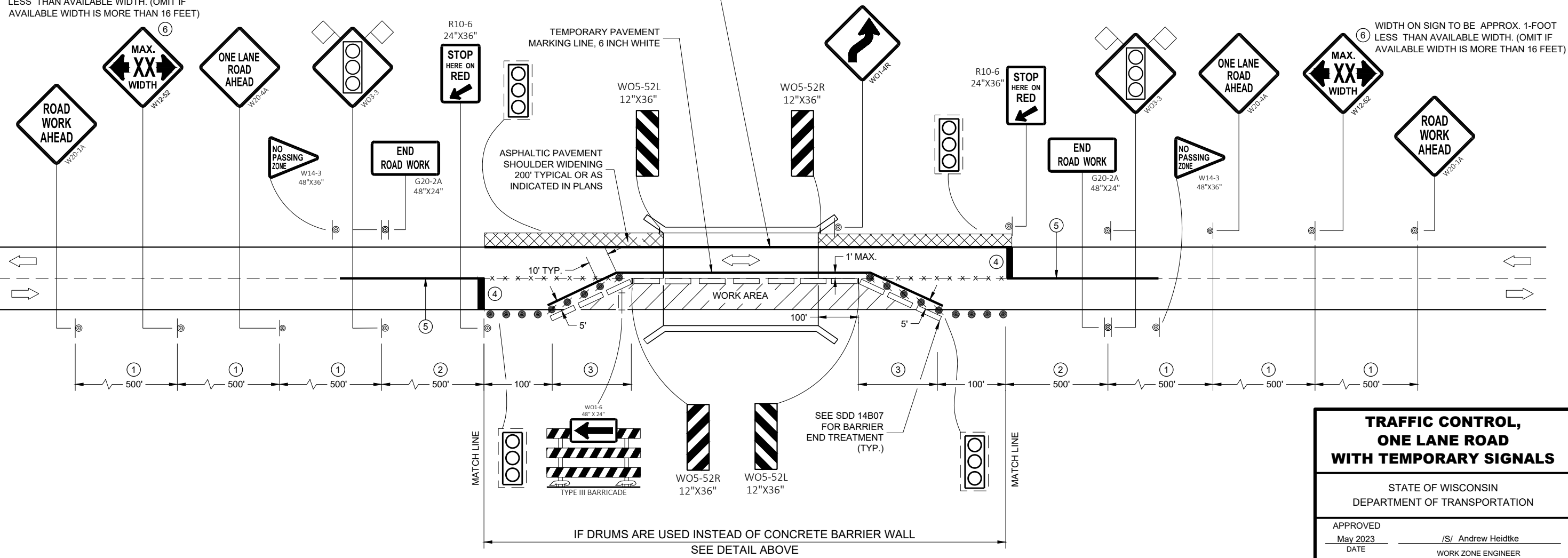
WIDTH ON SIGN TO BE APPROX. 1-FOOT LESS THAN AVAILABLE WIDTH. (OMIT IF AVAILABLE WIDTH IS MORE THAN 16 FEET)

GENERAL NOTES

- THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.
- THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE..
- THIS LANE CLOSURE IS TYPICAL FOR CLOSING RIGHT LANE - REVERSE FOR CLOSING LEFT LANE.
- ALL SIGNS ARE 48" x 48" UNLESS OTHERWISE NOTED.
- "WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.
- ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED OR AS APPROVED BY THE ENGINEER.
- REMOVE PAVEMENT MARKING AND PLACE TEMPORARY PAVEMENT MARKING LINES IF THE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.
- INSTALL OVERHEAD TEMPORARY SIGNAL HEADS ABOVE THE MIDDLE OF THE TRAVEL LANE THEY ARE CONTROLLING.
- ① 500 FOOT SPACING SHOWN IS FOR ROADWAYS WITH A PRE-CONSTRUCTION REGULATORY SPEED LIMIT OF 45 MPH OR MORE. FOR 35 - 40 MPH, USE 350 FOOT TYPICAL SPACING. FOR 25 - 30 MPH, USE 200 FOOT TYPICAL SPACING.
 - ② USE 300 FOOT SPACING IF THE PRE - CONSTRUCTION REGULATORY SPEED IS 35 MPH OR LESS.
 - ③ DIMENSION DETERMINED BY CBTP TAPER FROM EDGE LINE TO TANGENT SECTION OF THE ROAD.
 - ④ TEMPORARY PAVEMENT MARKING LINE, 18 INCH WHITE STOP LINE.
 - ⑤ 700 FOOT TEMPORARY PAVEMENT MARKING LINE, 6 INCH DOUBLE YELLOW . WHEN THE DISTANCE FOR THE PRECEDING NO - PASSING ZONE IS LESS THAN THE MINIMUM DISTANCE BETWEEN ZONES AS INDICATED IN THE SPECIFICATIONS, THE TWO ZONES SHALL BE CONNECTED.
 - ⑥ SEE SDD 15C02 - SHEET "F" FOR ADVANCED WIDTH RESTRICTION SIGNING.



TEMPORARY PAVEMENT MARKING LINE, 6 INCH WHITE (STOPLINE TO STOPLINE). REMOVE EXISTING EDGELINE AND OFFSET THE TEMPORARY EDGELINE IF THE DISTANCE FROM THE EDGELINE TO CONCRETE BARRIER WALL IS LESS THAN 9 FEET.

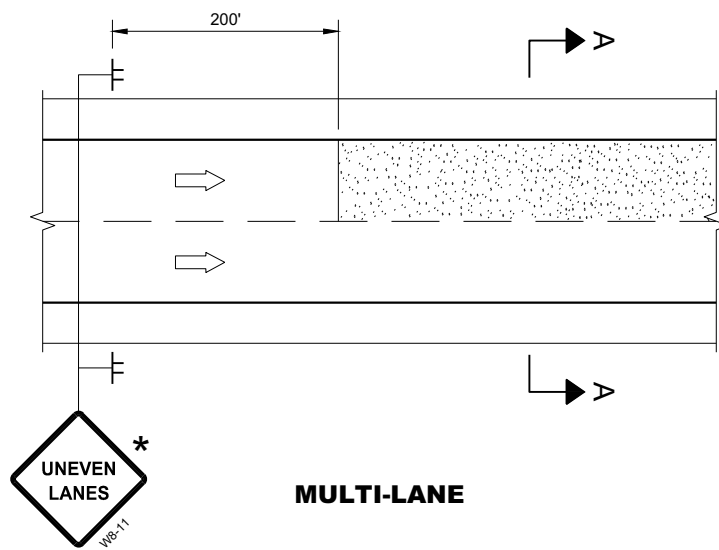


**TRAFFIC CONTROL,
ONE LANE ROAD
WITH TEMPORARY SIGNALS**

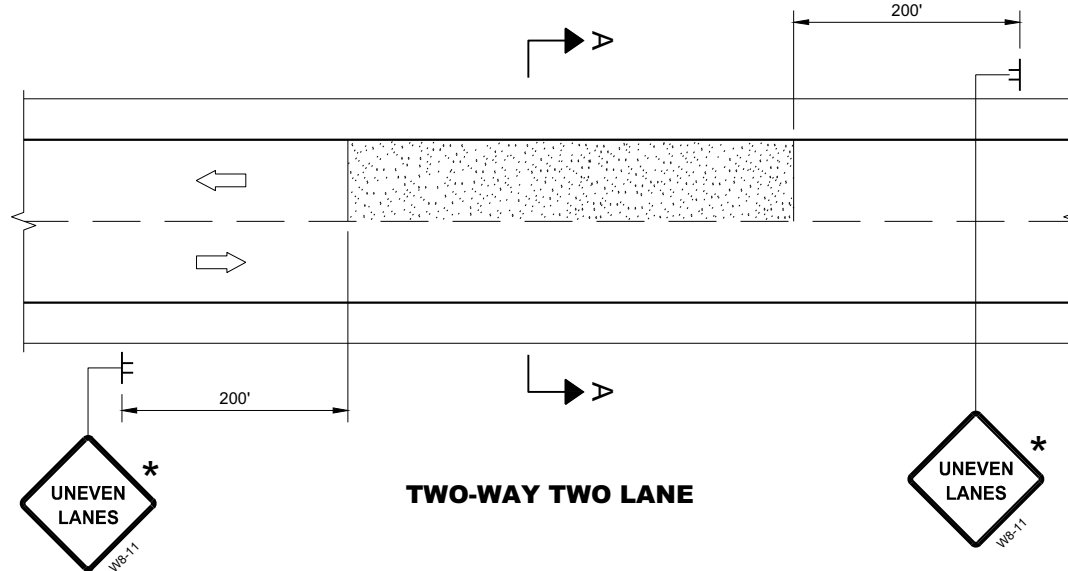
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED _____ /S/ Andrew Heidtke
DATE May 2023 WORK ZONE ENGINEER

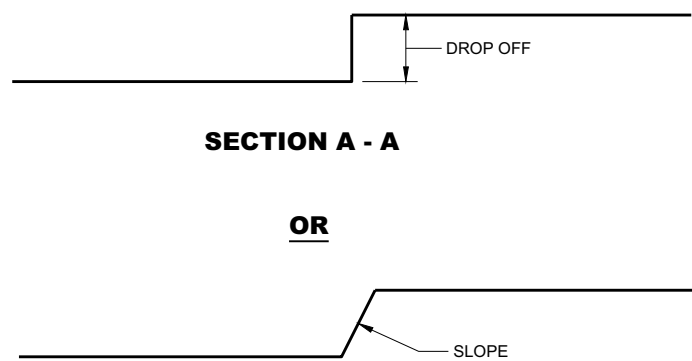
FHWA



MULTI-LANE



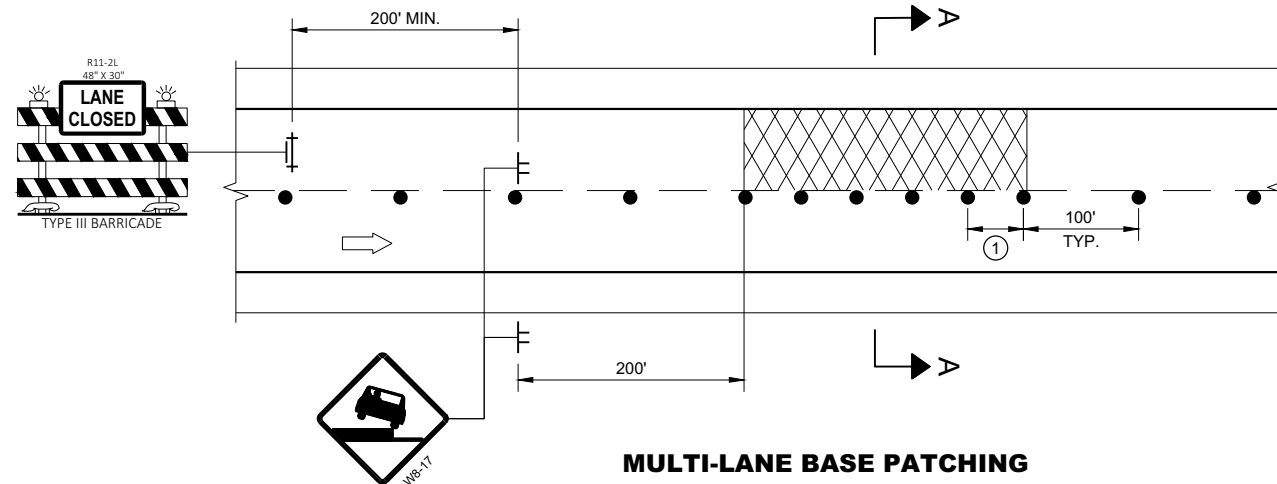
TWO-WAY TWO LANE



SECTION A - A

OR

SECTION A - A



MULTI-LANE BASE PATCHING

ADJACENT LANE DROP-OFFS

GENERAL NOTES

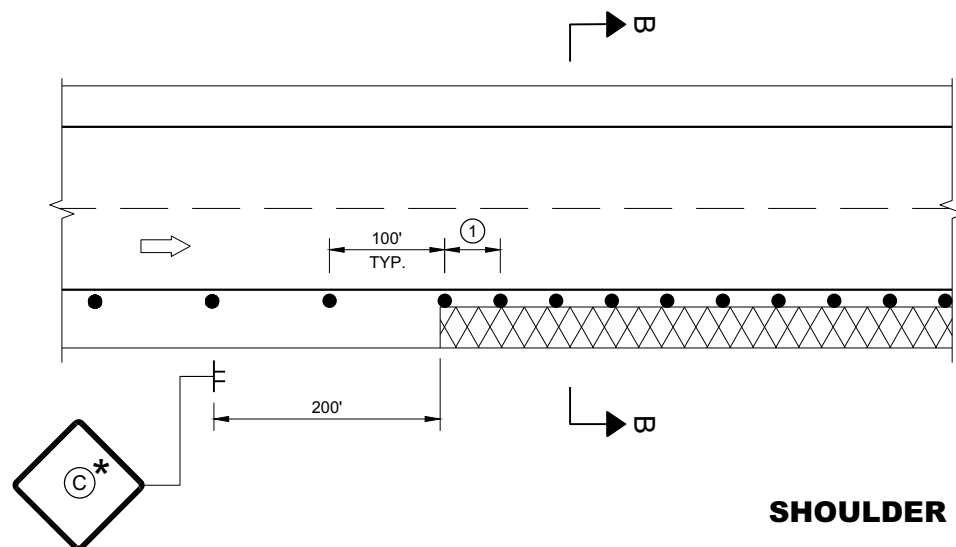
- FOR SPOT LOCATIONS USE ENGINEERING JUDGEMENT WHEN PLACING ADDITIONAL SIGNS.
- ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.
- "WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.
- WARNING LIGHTS ARE NOT REQUIRED IF THE LANE CLOSURE IS A DAYTIME ONLY OPERATION.
- * IF THE DROP-OFF IS CONTINUOUS ALONG THE PROJECT, PLACE ADDITIONAL SIGNS EVERY 1 MILE AND AFTER EVERY ENTRANCE RAMP.
- ① USE CLOSER SPACING WHEN DELINEATING DROP-OFF.

LEGEND

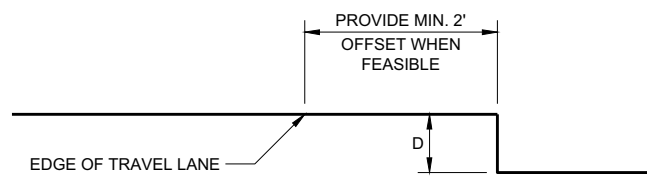
- SIGN ON TEMPORARY SUPPORT
- TRAFFIC CONTROL DRUM
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- DIRECTION OF TRAFFIC
- WORK AREA WITH DROP-OFF
- MILLED SURFACE

6

6



SHOULDER DROP-OFFS



SECTION B - B

D	SIGN (C)
< 2" WITH A SLOPE STEEPER THAN 3:1	LOW SHOULDER WO8-9
2" < 6" WITH A SLOPE STEEPER THAN 3:1	SHOULDER DROP - OFF W8-9A PROVIDE A 3:1 OR FLATTER SLOPE OF MATERIAL ADJACENT TO THE PAVEMENT

SDD 15D39 - 02

SDD 15D39 - 02

**TRAFFIC CONTROL,
DROP-OFF SIGNING**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
March 2018 /S/ Andrew Heidtke
DATE DATE WORK ZONE ENGINEER

FHWA

GENERAL NOTES

DRAWING NOT TO SCALE. ALL SIGNS AND POSTS ON THIS SHEET SHALL BE PAID FOR WITH 'TRAFFIC CONTROL SIGNS' BID ITEM. ALL SIDE ROADS WHICH ARE UNDER CONSTRUCTION OF CURB AND GUTTER AND/OR GRADING SHALL BE ADEQUATELY SIGNED.

ALL SIGNS AND DEVICES SHALL BE IN CONFORMANCE WITH THE WISCONSIN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD). SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISDOT STANDARD SIGN PLATES.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THAT THE BACKGROUND IS ORANGE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

ALL SIGNS INAPPROPRIATE TO THE STATUS OF THE CONTROL ZONE, INCLUDING PRE-EXISTING SIGNS IN THE VICINITY, SHALL BE COVERED OR REMOVED AS DIRECTED BY THE ENGINEER.

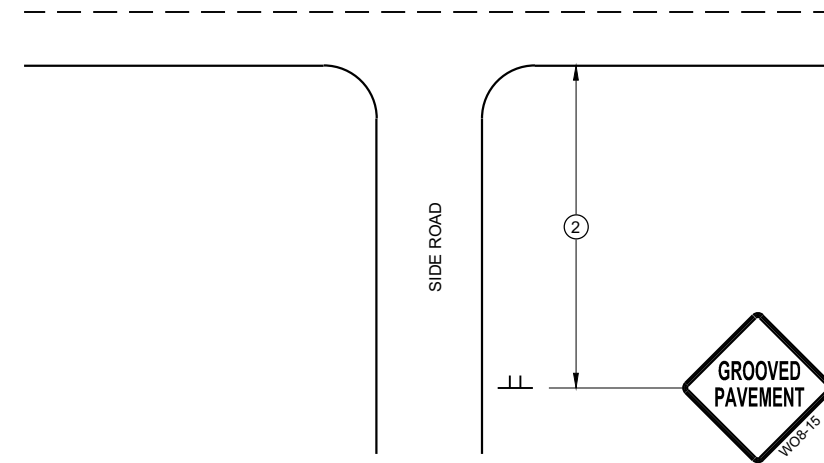
SEE 15C34 FOR ADDITIONAL TRAFFIC CONTROL SIGNING WHEN CENTERLINE PAVEMENT MAKINGS ARE MISSING. 'DO NOT PASS' SIGNS MUST BE INSTALLED ON THE SAME DAY AS MILLING OPERATIONS.

- ① PLACE SIGNS 350' IN ADVANCE OF MILLED SURFACES AND AT 1 MILE INTERVALS, OR AS DIRECTED BY THE ENGINEER.
- ② PLACE SIGN 200' MIN. FROM INTERSECTION AND 200' MIN. AFTER ADVANCE WARNING SIGN SHOWN IN SDD 15C04.

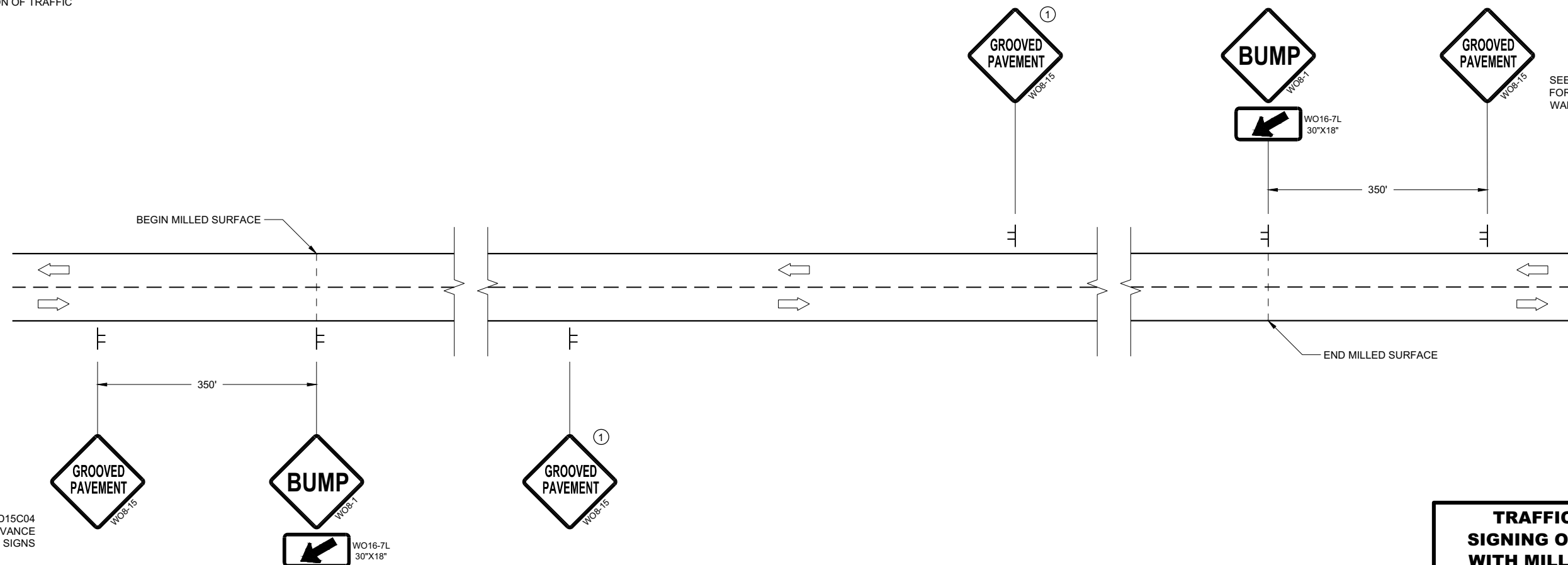
LEGEND

⊥ SIGN ON TEMPORARY SUPPORT

➡ DIRECTION OF TRAFFIC



TYPICAL SIDE ROAD APPROACH SIGN DETAIL



DETAIL FOR SIGNING ON MILLED SURFACES

SEE SDD15C04 FOR ADVANCE WARNING SIGNS

TRAFFIC CONTROL, SIGNING ON ROADWAYS WITH MILLED SURFACES

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2020 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA

GENERAL NOTES

DRAWING NOT TO SCALE. ALL SIGNS AND POSTS ON THIS SHEET SHALL BE PAID FOR WITH 'TRAFFIC CONTROL SIGNS' BID ITEM. ALL SIDE ROADS WHICH ARE UNDER CONSTRUCTION OF CURB AND GUTTER AND/OR GRADING SHALL BE ADEQUATELY SIGNED.

ALL SIGNS AND DEVICES SHALL BE IN CONFORMANCE WITH THE WISCONSIN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (WMUTCD). SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISDOT STANDARD SIGN PLATES.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THAT THE BACKGROUND IS ORANGE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

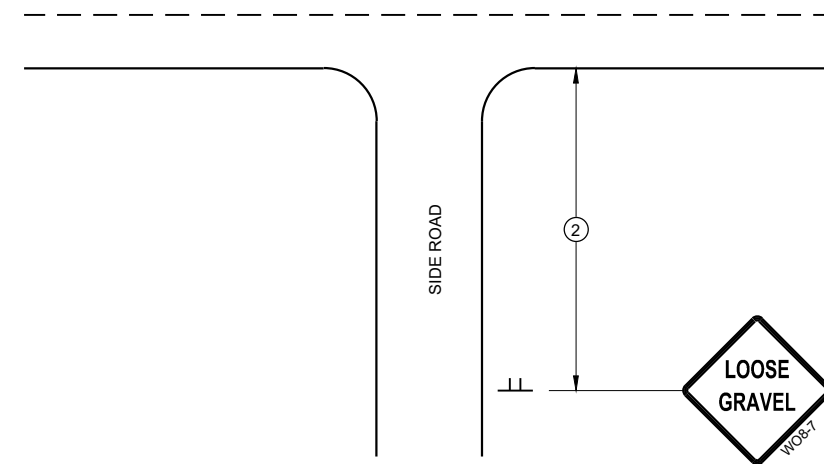
ALL SIGNS INAPPROPRIATE TO THE STATUS OF THE CONTROL ZONE, INCLUDING PRE-EXISTING SIGNS IN THE VICINITY, SHALL BE COVERED OR REMOVED AS DIRECTED BY THE ENGINEER.

SEE 15C34 FOR ADDITIONAL TRAFFIC CONTROL SIGNING WHEN CENTERLINE PAVEMENT MAKINGS ARE MISSING. 'DO NOT PASS' SIGNS MUST BE INSTALLED ON THE SAME DAY AS MILLING OPERATIONS.

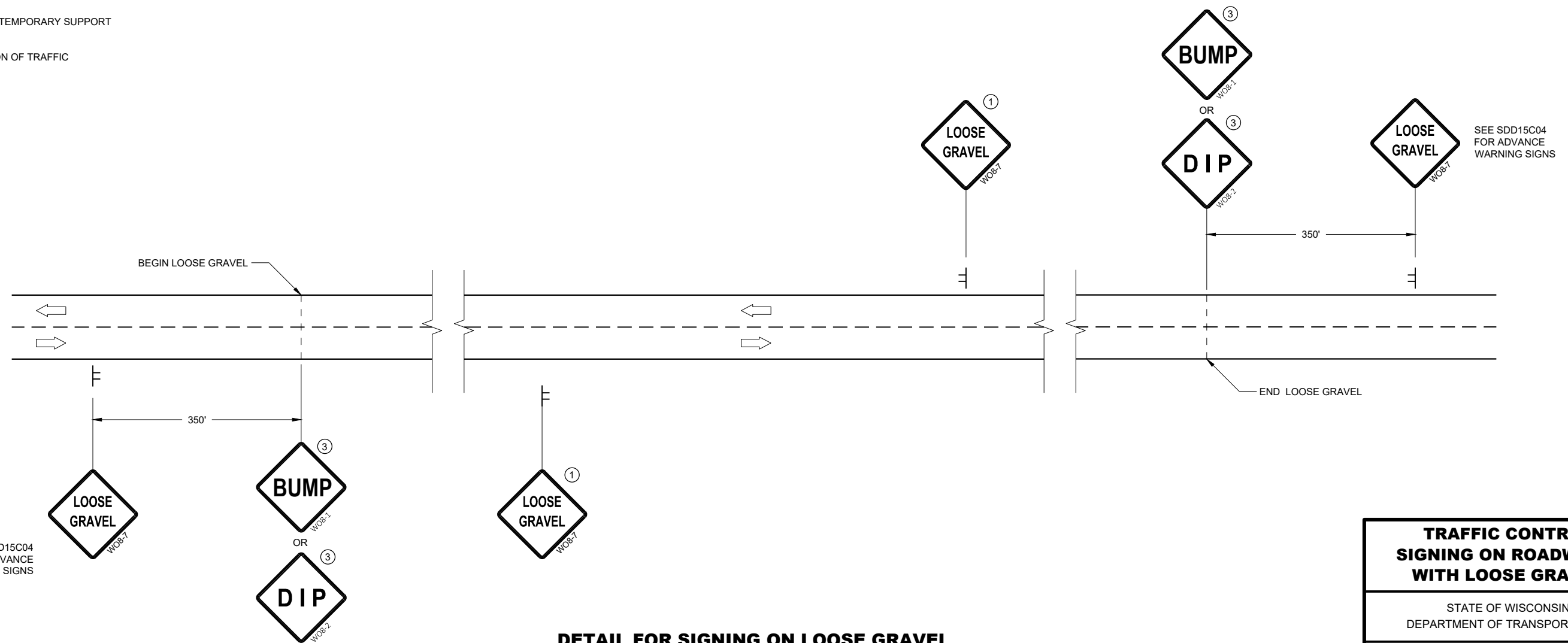
- ① PLACE SIGNS 350' IN ADVANCE OF CHIP SEALED OR LOOSE GRAVEL SURFACES AND AT 1 MILE INTERVALS, OR AS DIRECTED BY THE ENGINEER.
- ② PLACE SIGN 200' MIN. FROM INTERSECTION AND 200' MIN. AFTER ADVANCE WARNING SIGN SHOWN IN SDD 15C04.
- ③ ADD WO8-1 OR WO8-2 SIGN WHEN THE CONDITION IS PRESENT.

LEGEND

- ⌋ SIGN ON TEMPORARY SUPPORT
- ➡ DIRECTION OF TRAFFIC



TYPICAL SIDE ROAD APPROACH SIGN DETAIL



DETAIL FOR SIGNING ON LOOSE GRAVEL OR CHIP SEALED SURFACES

SEE SDD15C04 FOR ADVANCE WARNING SIGNS





TRAFFIC CONTROL SIGNING ON ROADWAYS WITH LOOSE GRAVEL

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2021 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

FHWA

LEGEND

- V1 WORK VEHICLE
- V2 SHADOW VEHICLE
-  TRUCK MOUNTED ATTENUATOR (TMA)
-  FLASHING ARROW PANEL (CAUTION)
-  WORK AREA
-  DIRECTION OF TRAFFIC

POSTED SPEED PRIOR TO WORK STARTING (MPH)	DECISION SIGHT DISTANCE (D)
0 - 25	550'
30	550'
35	700'
40	700'
45	900'
50	900'
55	1200'

GENERAL NOTES

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED.

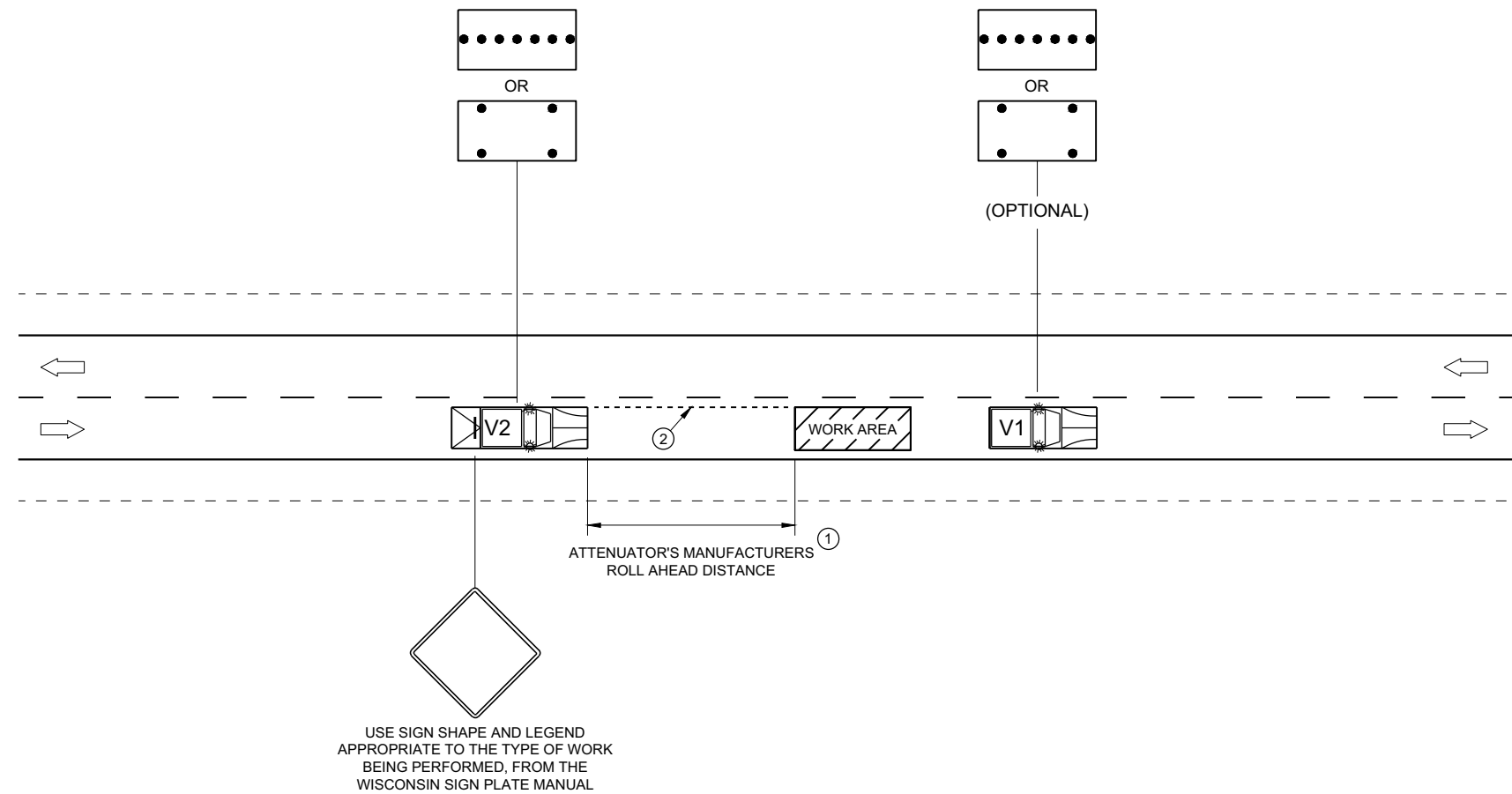
MOBILE IS WORK THAT MOVES CONTINUOUSLY OR MOVES AT LEAST THE DECISION SIGHT DISTANCE EVERY 15 MINUTES.

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

ALL ARROW PANELS SHALL BE REAR FACING, TYPE "B" OR "C", AND DISPLAYING THE FLASHING CAUTION MODE. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

USE AN ATTENUATOR ON THE REARMOST VEHICLE THAT BLOCKS ALL OR PART OF THE TRAFFIC LANE.

- ① DISTANCE BETWEEN VEHICLES MAY INCREASE FROM THE ATTENUATOR'S ROLL AHEAD BASED ON TERRAIN, SIGHT DISTANCE, AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL OR HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.
- ② ALIGN LEFT SIDE OF SHADOW VEHICLE WITH EDGE OF WORK AREA.



USE SIGN SHAPE AND LEGEND APPROPRIATE TO THE TYPE OF WORK BEING PERFORMED, FROM THE WISCONSIN SIGN PLATE MANUAL

**TRAFFIC CONTROL,
MOBILE OPERATIONS ON
AN UNDIVIDED ROADWAY**

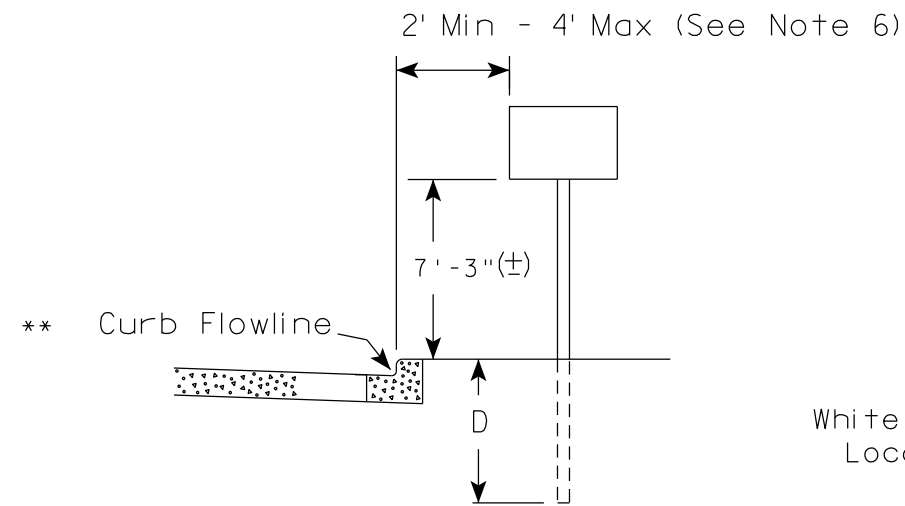
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
February 2021 DATE /S/ Andrew Heidtke
STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER

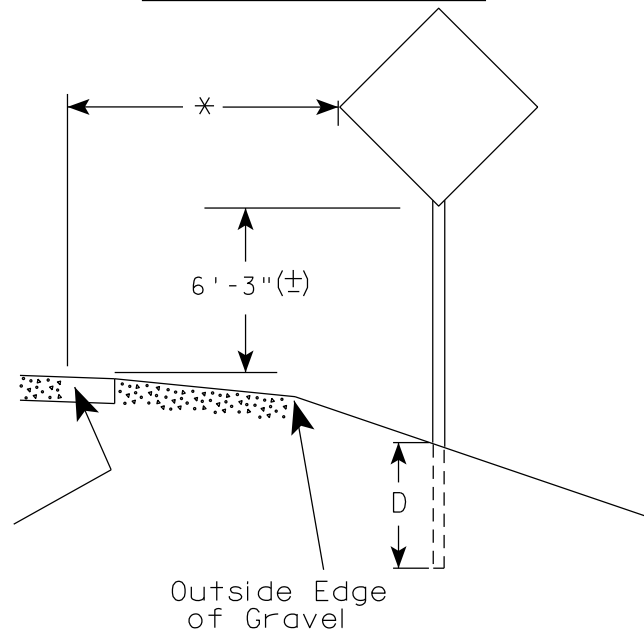
FHWA

URBAN AREA

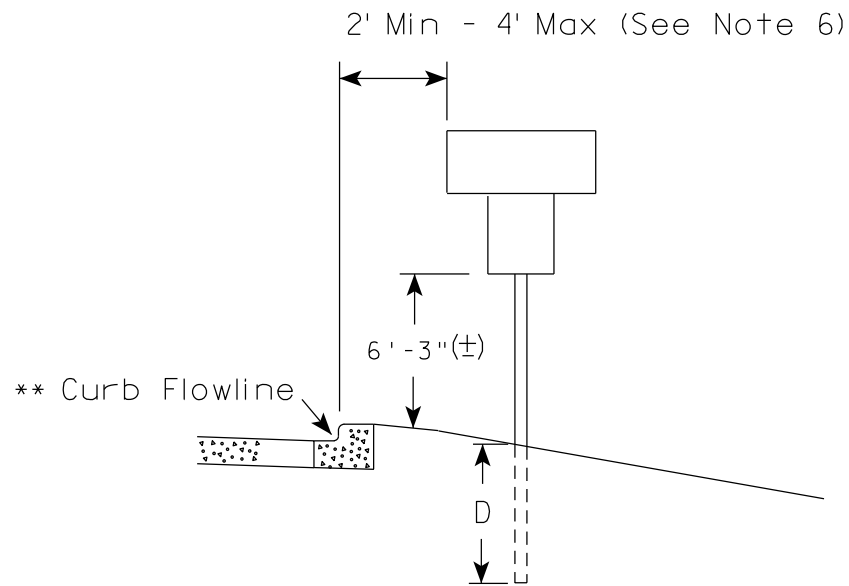
RURAL AREA (See Note 2)



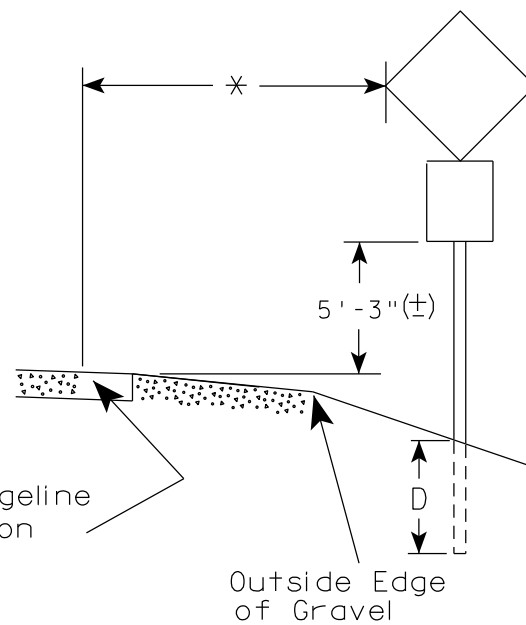
White Edgeline Location



Outside Edge of Gravel



White Edgeline Location



Outside Edge of Gravel

POST EMBEDMENT DEPTH

Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

GENERAL NOTES

1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on or behind barrier wall, see A4-10 sign plate.
The Double Arrow sign (W12-1D) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).
3. For expressways and freeways, mounting height is 7'- 3" (±) or 6'-3" (±) depending upon existence of a sub-sign.
4. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (±).
5. Offset distance shall be consistent with existing signs or consistent throughout length of project.
6. The (±) tolerance for mounting height is 3 inches.
7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the Engineer.

* * The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

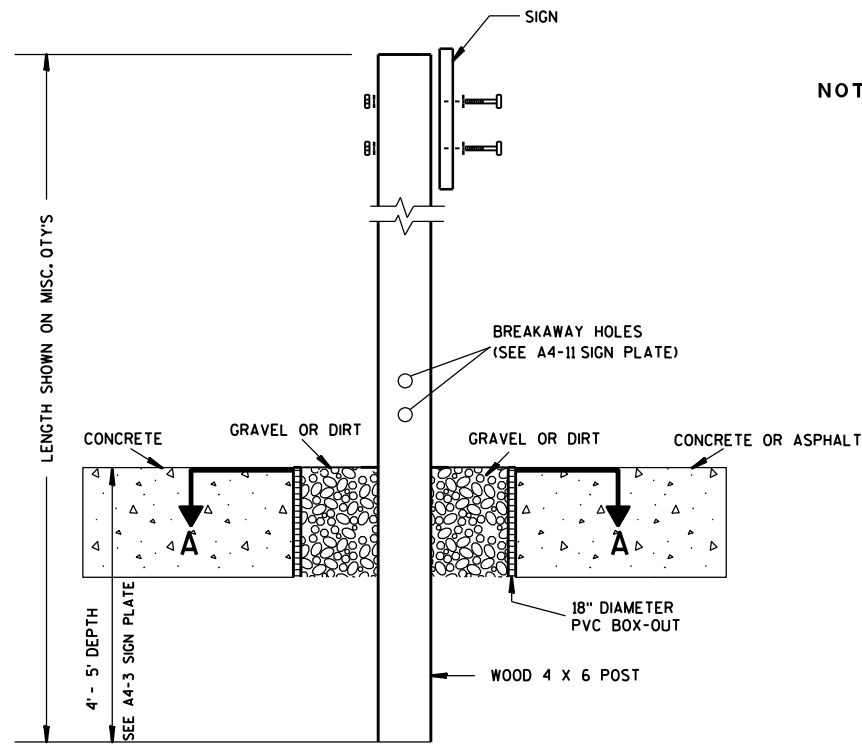
* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

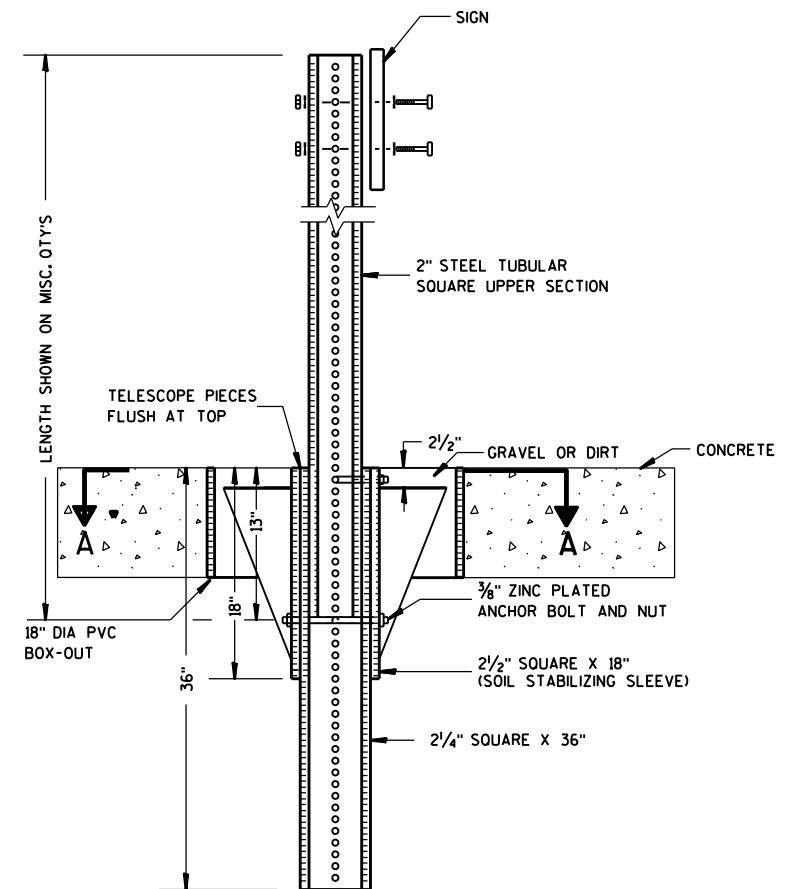
DATE 5/13/2020 PLATE NO. A4-3.22



ELEVATION VIEW

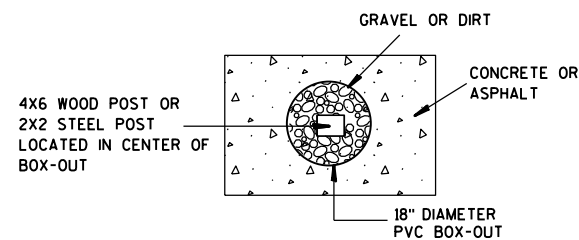
DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

- NOTES:**
1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
 2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
 3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



PLAN VIEW

FOR NEW CONCRETE/ ASPHALT INSTALLATIONS

**SIGN POST
BOX-OUTS
A4-3B**

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 1/27/14 PLATE NO. A4-3B.1

PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E

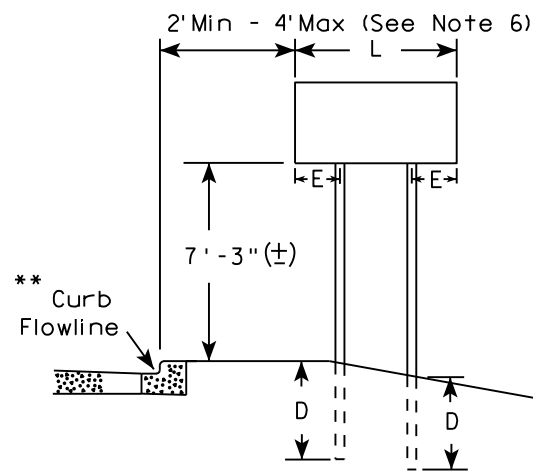
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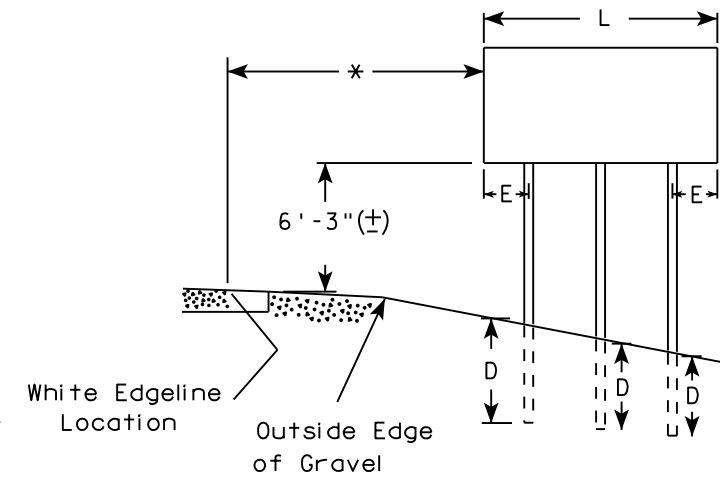
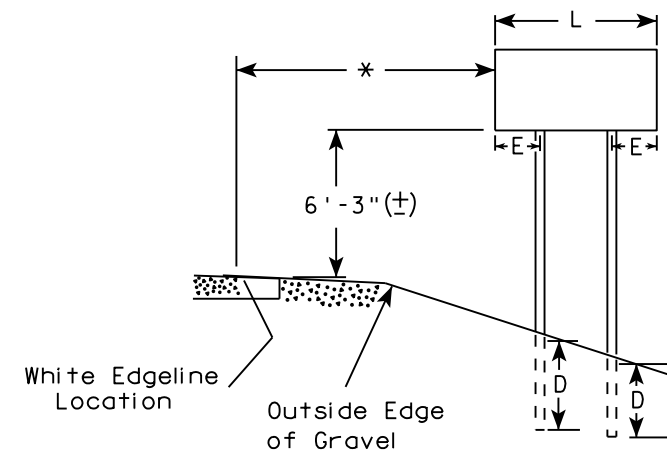
GENERAL NOTES

1. For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
2. See tables below for required number of posts.
3. For expressways and freeways, mounting height is 7'-3" (±) or 6'-3" (±) depending upon existence of sub-sign.
4. The (±) tolerance for mounting height is 3 inches.
5. J-Assemblies are considered to be one sign for mounting height.
6. Offset distance shall be consistent with existing signs or consistent throughout length of project.
7. Folding signs shall be mounted at a height of 5'-3" (±) or as directed by the engineer.
8. The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (±). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (±).

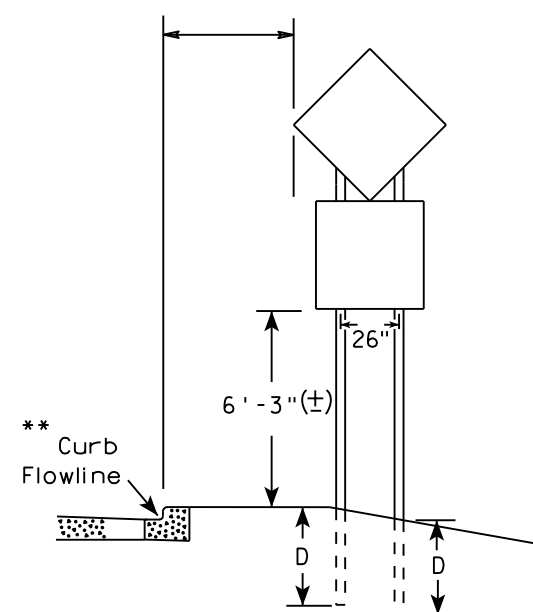
URBAN AREA



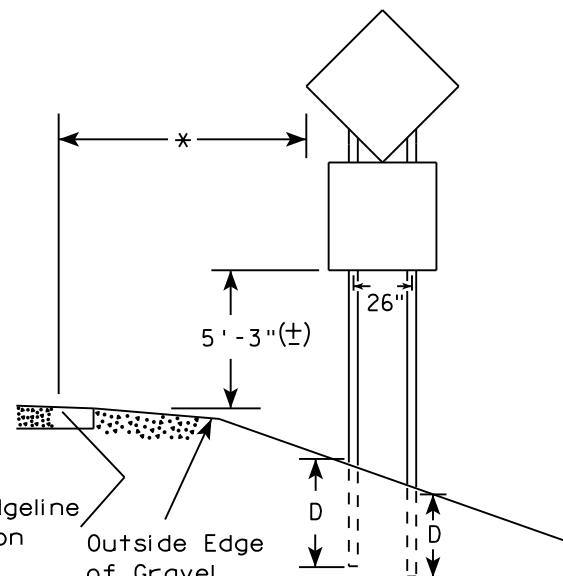
RURAL AREA (See Note 3)



2' Min - 4' Max (See Note 6)



48" DIAMOND WARNING SIGN



48" DIAMOND WARNING SIGN

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

*** See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED)	
L	E
Greater than 48" Less than 60"	12"
60" to 108"	L/5

SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED)	
L	E
Greater than 108" to 144"	12"

POST EMBEDMENT DEPTH

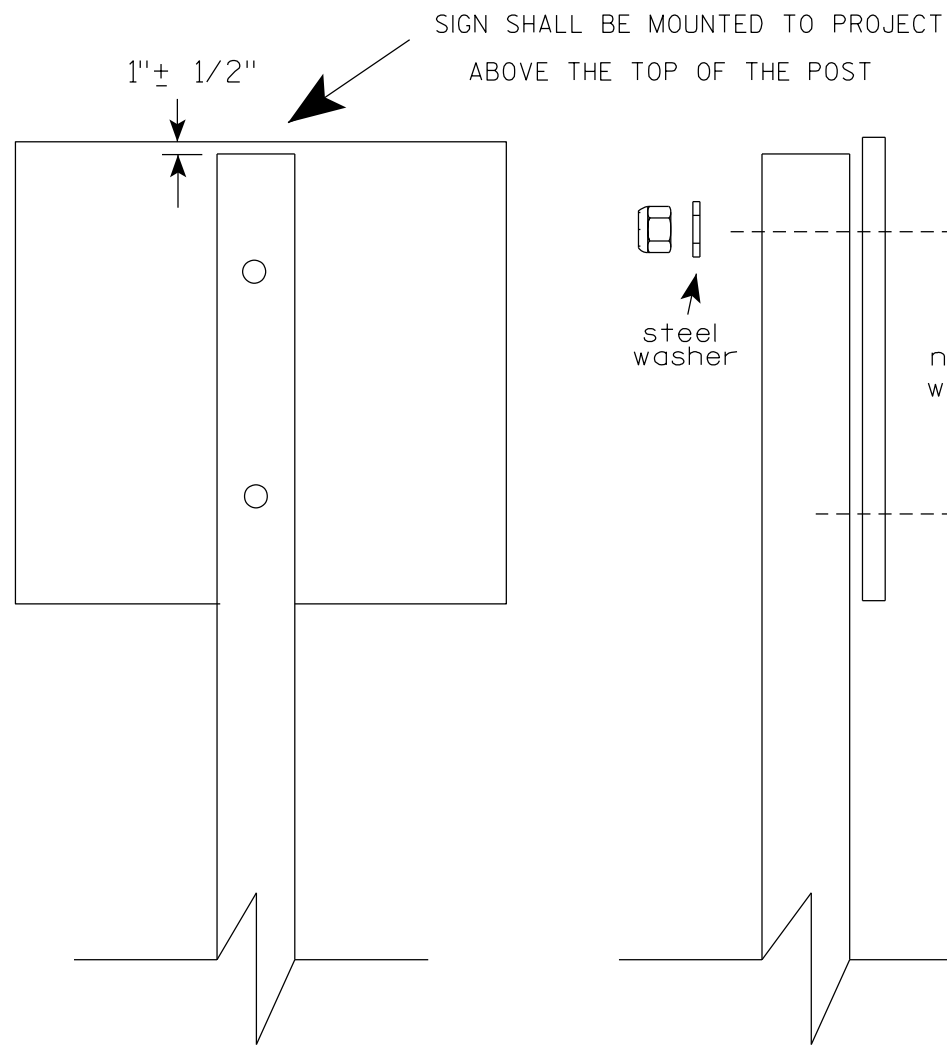
Area of Sign Installation (Sq. Ft.)	D (Min)
20 or Less	4'
Greater than 20	5'

TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 8/21/17 PLATE NO. A4-4.15



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

- Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
- Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

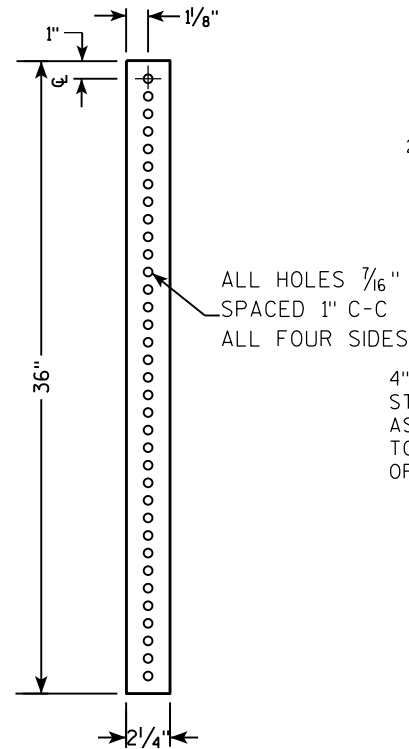
- STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)
- MACHINE BOLTS - $\frac{5}{16}$ " X 1-3/4" Length w/ lock nuts
- WOOD POSTS (4" x 6")
- LAG SCREWS - $\frac{3}{8}$ " X 3" (NO STRINGERS ON BACK OF SIGN)
 - $\frac{3}{8}$ " X 4" (STRINGERS ON BACK OF SIGN)
- SQUARE STEEL POSTS (2" x 2")
- MACHINE BOLTS - $\frac{3}{8}$ " X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)
 - $\frac{3}{8}$ " X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)
- RIVETS - $\frac{9}{32}$ " (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
- O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH
- WASHERS (ALL POSTS) -
- 1-1/4" O.D. X $\frac{3}{8}$ " I.D. X $\frac{1}{16}$ " STEEL
 - 1-1/4" O.D. X $\frac{3}{8}$ " I.D. X .080 NYLON

* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

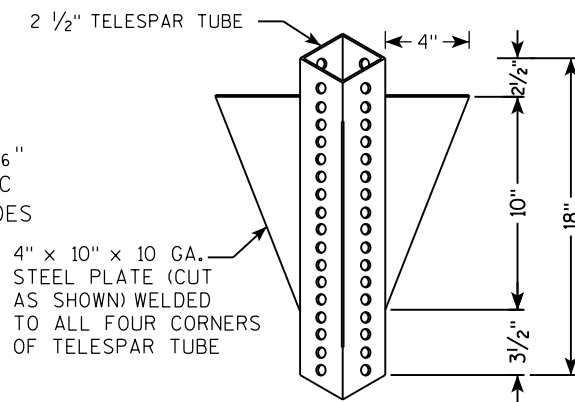
ATTACHMENT OF SIGNS TO POSTS	
WISCONSIN DEPT OF TRANSPORTATION	
APPROVED	<i>Matthew R Rauch</i> For State Traffic Engineer
DATE 4/1/2020	PLATE NO. A4-8.9

**TELESCOPIC TUBING ANCHORS
TWO PIECE SYSTEM**

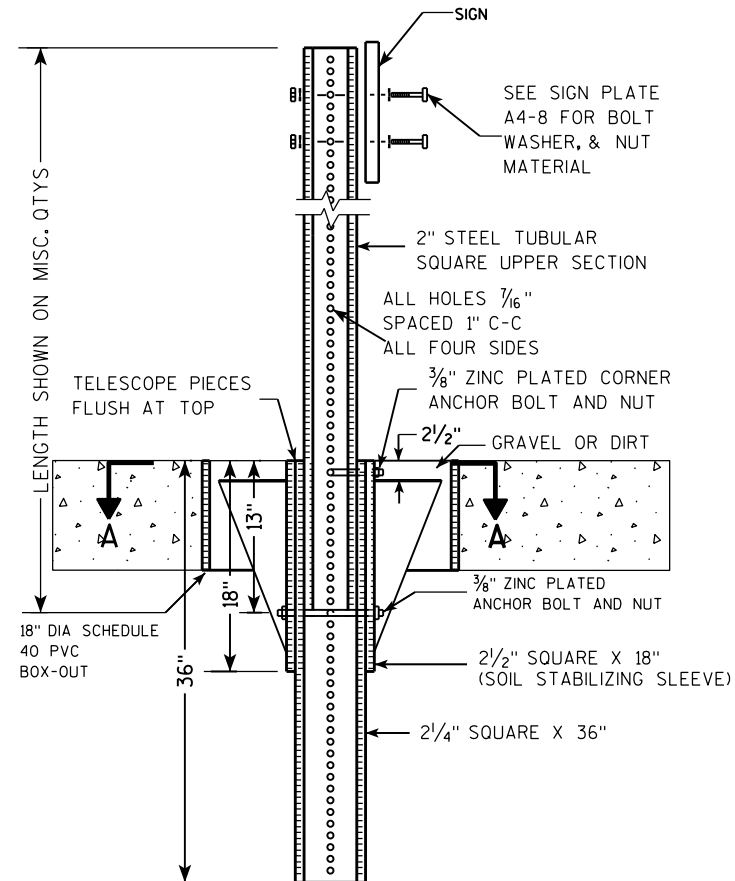
**2 1/4" SQUARE
12 GAUGE
PERFORATED
GALVANIZED FINISH**



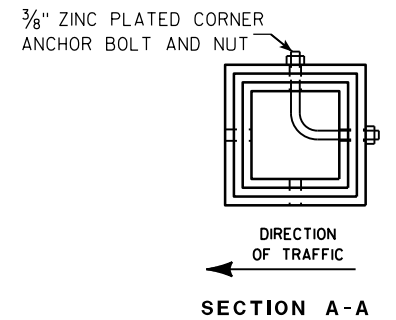
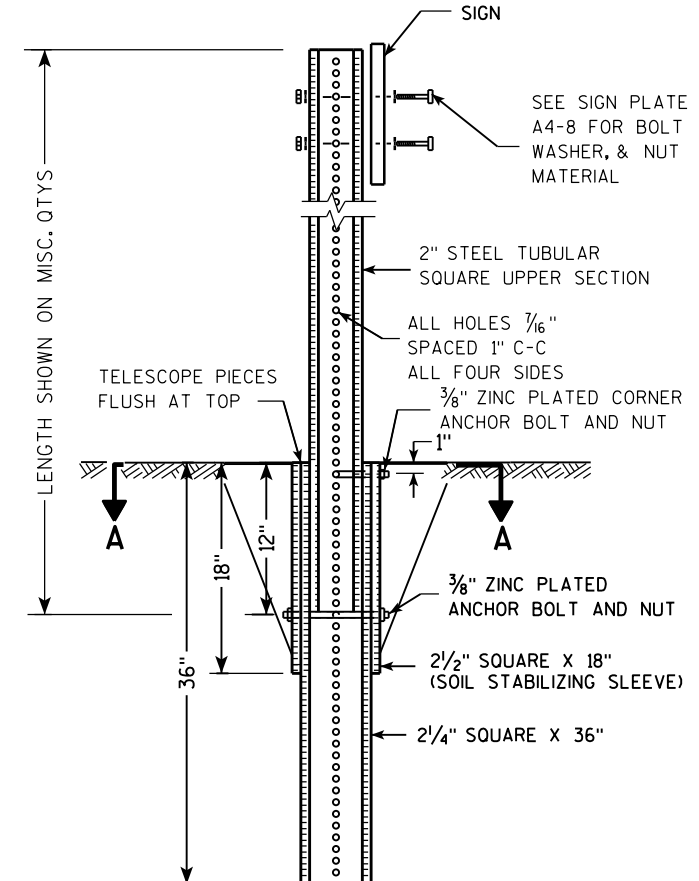
**2 1/2" SQUARE
12 GAUGE
OMNI-DIRECTIONAL
PERFORATED
SOIL STABILIZING SLEEVE
GALVANIZED FINISH**



**DETAIL OF TUBULAR STEEL SIGN POST
(IN POURED CONCRETE OR ASPHALT)**



**DETAIL OF TUBULAR STEEL SIGN POST
(IN LOCATIONS OTHER THAN POURED CONCRETE OR ASPHALT)**



Area of Sign Installation (Sq. Ft.)	Number of Required Posts
9 or less	1
Greater than 9 less than or equal to 18	2
Greater than 18 less than or equal to 27	3

Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

**TUBULAR STEEL
SIGN POST
A4-9**

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 2/05/15 PLATE NO. A4-9.9

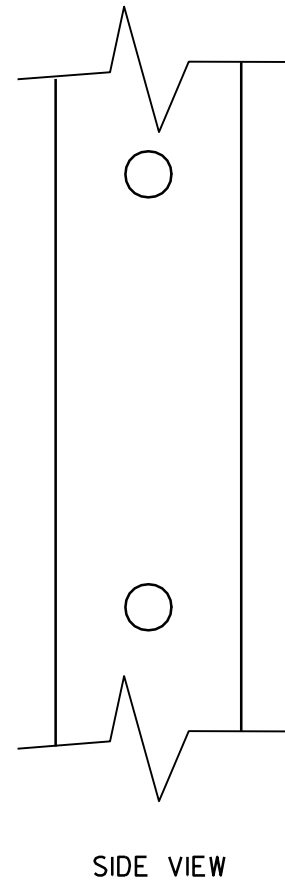
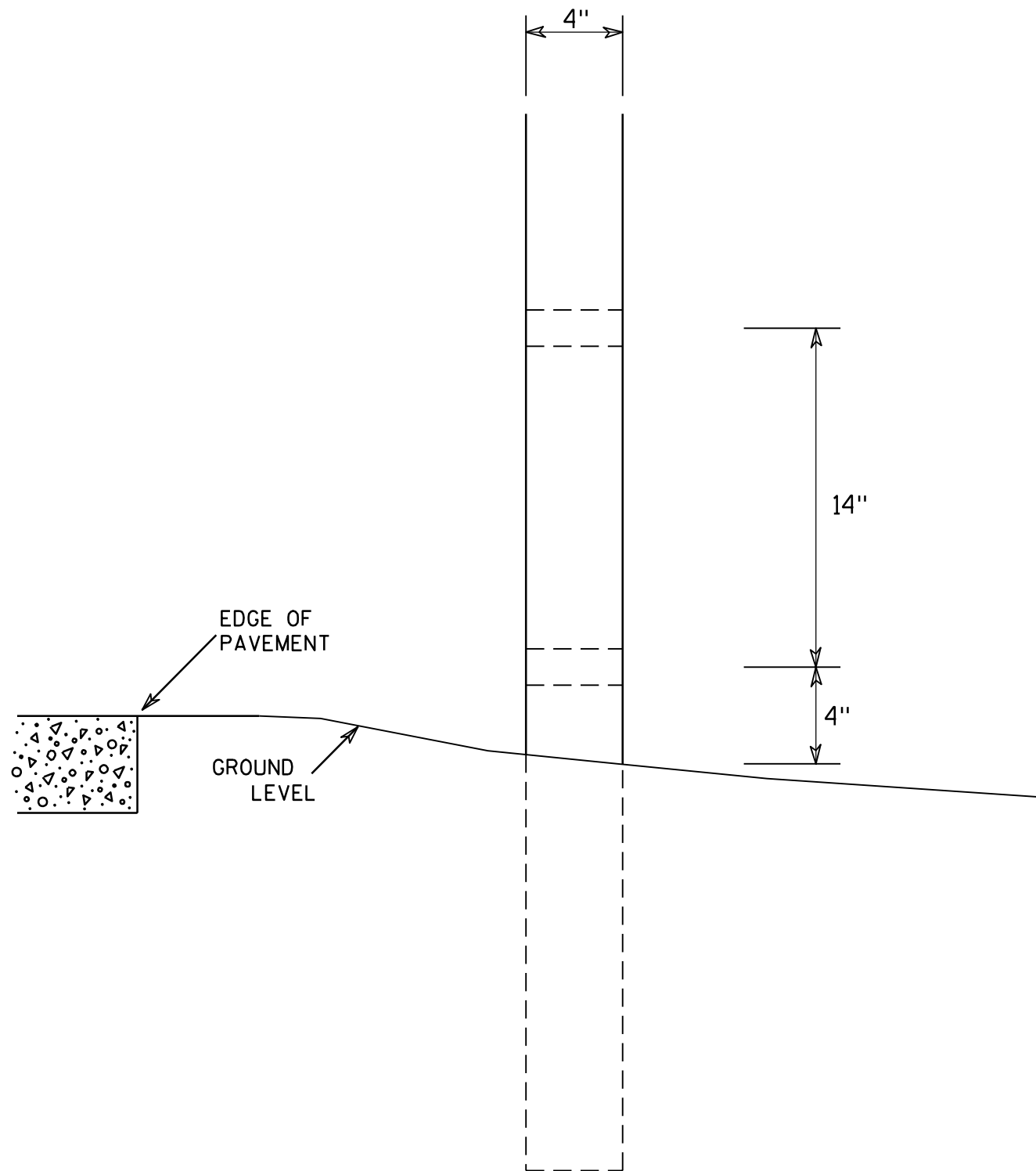
PROJECT NO:

HWY:

COUNTY:

SHEET NO:

E



GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

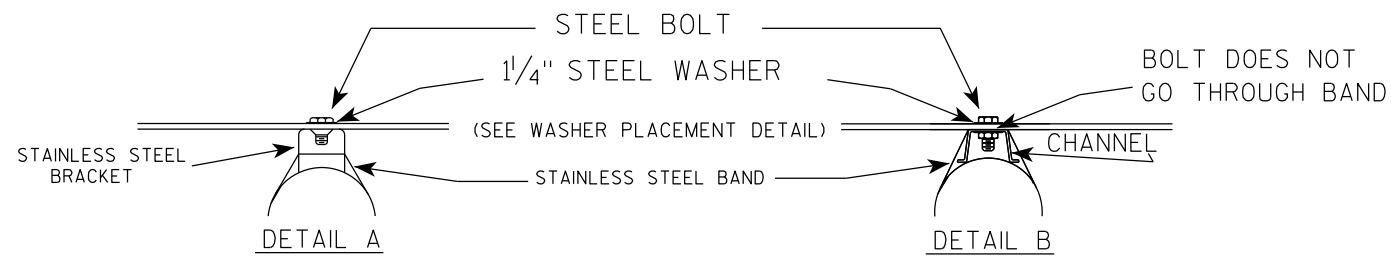
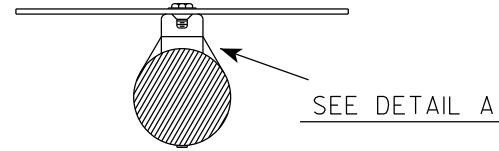
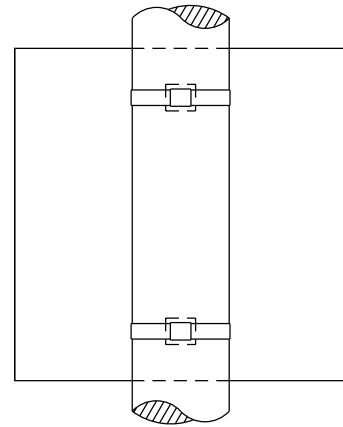
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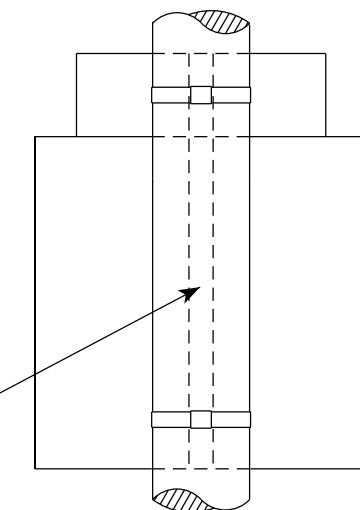
4 X 6 WOOD POST MODIFICATIONS	
<i>WISCONSIN DEPT OF TRANSPORTATION</i>	
APPROVED	<i>Chester J Spang</i> for State Traffic Engineer
DATE <u>3/27/97</u>	PLATE NO. <u>A4-11.2</u>

BANDING

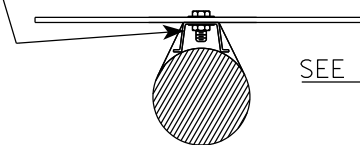
SINGLE SIGN



"J" ASSEMBLY

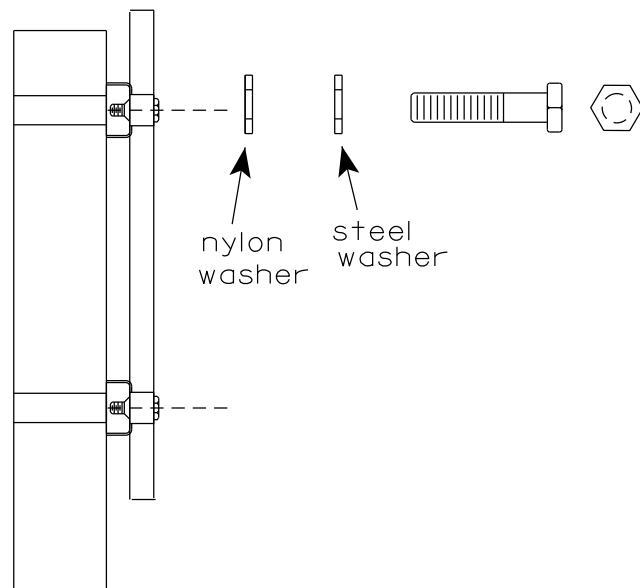


CHANNEL
SEE TYPICAL PANEL
INSTALLATION SHEET



- GENERAL NOTES**
1. Any sign over 3 feet in width shall use the V-Block banding method. See A5-10 standard plate.
 2. Signs 3 feet or greater in height shall have three bracket bands installed. Signs less than 3 feet in height shall have two bracket bands installed.
 3. Banding and assembly bracket shall be stainless steel. All bands shall be $\frac{3}{4}$ " in width and 0.025" thickness.
 4. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
 - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
 - b. Electro-galvanized in accordance with ASTM designation: B 633, Type III, SC 3

WASHER PLACEMENT



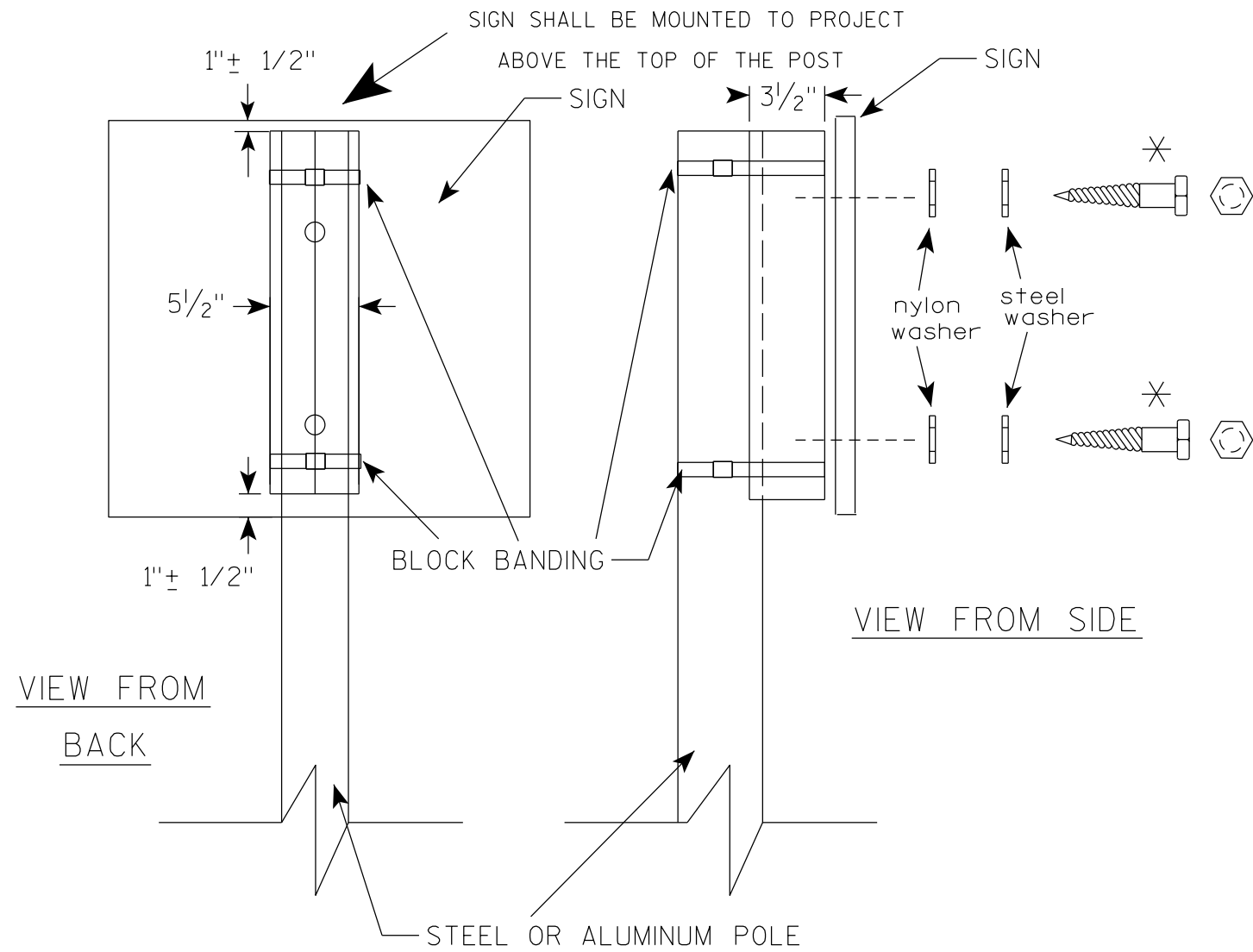
WASHERS (ALL POSTS) -
 1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
 1-1/4" O.D. X 3/8" I.D. X .080 NYLON
 FOR ALL TYPE H SIGNS

STANDARD SIGN
SIGN BANDING DETAILS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

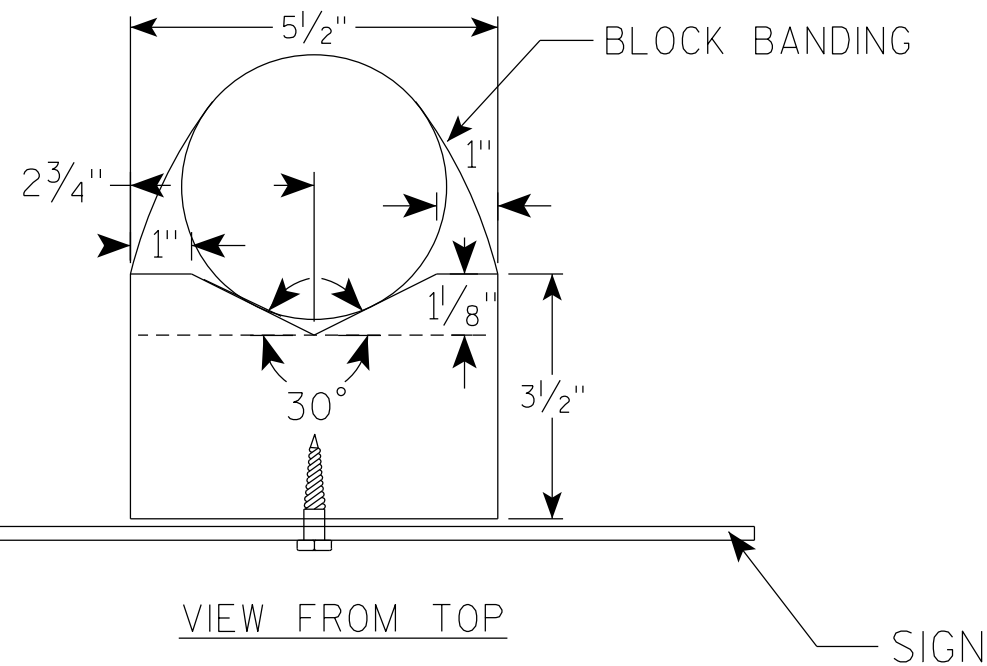
DATE 6/10/19 PLATE NO. A5-9.4



VIEW FROM
BACK

VIEW FROM SIDE

STEEL OR ALUMINUM POLE



VIEW FROM TOP

SIGN

GENERAL NOTES

1. WOOD 4"X6" POST MATERIAL SHALL CONFORM TO 507.2.2 OF THE WISDOT STANDARD SPECIFICATIONS
2. BLOCK BANDING AND CLIPS SHALL BE STAINLESS STEEL, 3/4" WIDTH AND 0.025" THICKNESS
3. SIGNS 3' OR GREATER IN HEIGHT SHALL UTILIZE 3 BLOCK BANDS. SIGNS UNDER 3' IN HEIGHT SHALL UTILIZE 2 BLOCK BANDS
4. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA, BUT NORMALLY THERE ARE TWO. FOR SIGNS GREATER THAN 9 S.F. 3 FASTENERS SHALL BE USED.
5. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
 - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
 - b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3
6. ALL BOLTS SHALL HAVE HEXAGONAL HEADS.
7. STEEL WASHERS SHALL BE 1/4" O.D. X 3/8" I.D. X 1/16"
8. NYLON WASHERS SHALL BE 1/4" O.D. X 3/8" I.D. X .080 FOR TYPE H OR TYPE F FACE SIGN

* LAG BOLTS SHALL BE 3/8" X 2 1/2"

BLOCK BANDING DETAIL
(V-BLOCK OPTION)

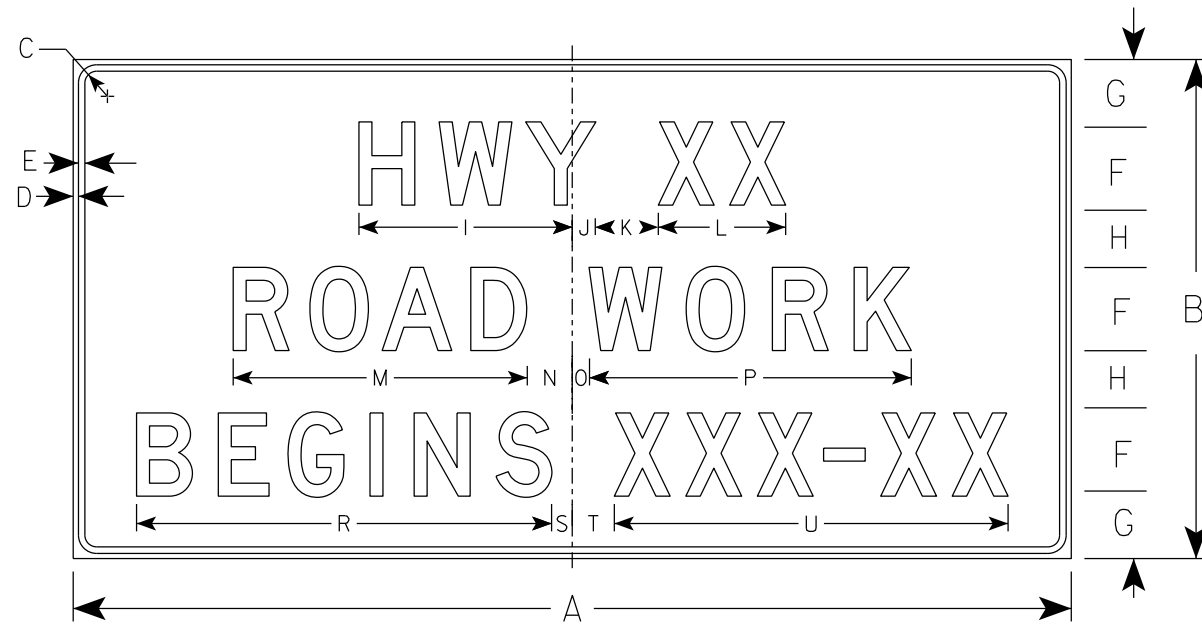
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 4/19/2022 PLATE NO. A5-10.3

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - D
4. Substitute appropriate numeral and adjust spacing to achieve proper balance.



G20-57

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2																											
3	72	36	1 1/8	1/2	5/8	6	5	4	15 5/8	1 5/8	5	9 1/4	21 1/4	3 1/2	1 1/2	23 1/4		29 7/8	1 3/4	3 1/4	28 1/2						18.0
4	96	48	2 1/4	3/4	1	8	6 1/2	5 1/2	20 5/8	2 1/4	6	12 1/4	28 1/4	4 3/8	1 5/8	31		39 1/4	2	4	37 7/8						32.0
5																											

STANDARD SIGN
G20-57

WISCONSIN DEPT OF TRANSPORTATION

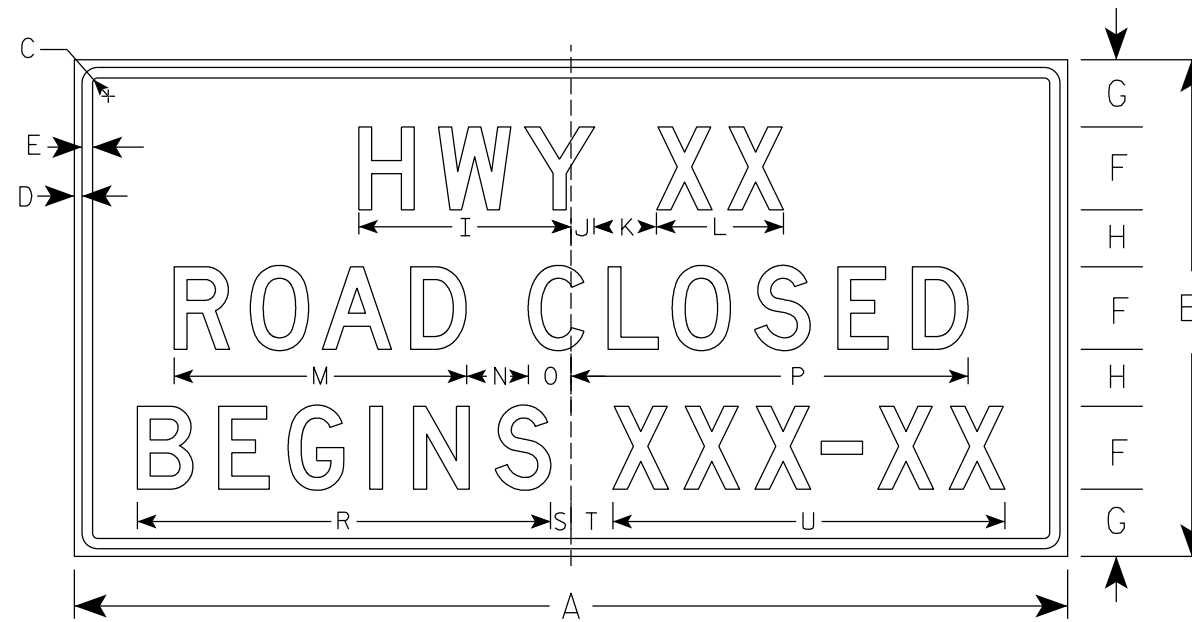
APPROVED *Matthew R Rauch*
For State Traffic Engineer

DATE 1/22/19 PLATE NO. G20-57.3

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

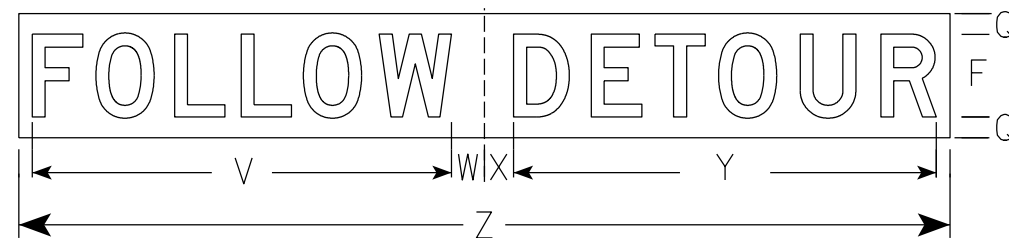
NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - D
4. Substitute appropriate numeral and adjust spacing to achieve proper balance.



G20-57C

PLAQUE ON .040" ALUMINUM



USE ONLY ONCE WHEN ROAD IS CLOSED

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2																											
3	72	36	1 1/8	1/2	5/8	6	5	4	15 5/8	1 5/8	5	9 1/4	21 1/8	5	2 7/8	29	2	30	1 3/4	3 1/4	28 3/8	40 1/2	2	2	29 3/4	66	18.0
4	96	48	2 1/4	3/4	1	8	6 1/2	5 1/2	20 5/8	2 1/4	6	12 1/4	28 1/4	6	4 1/8	38 3/8	2	39 7/8	2	4	37 7/8	29 3/4	3 1/8	2 7/8	40 7/8	90	32.0
5																											

STANDARD SIGN

G20-57C

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matthew R. Rauch
For State Traffic Engineer

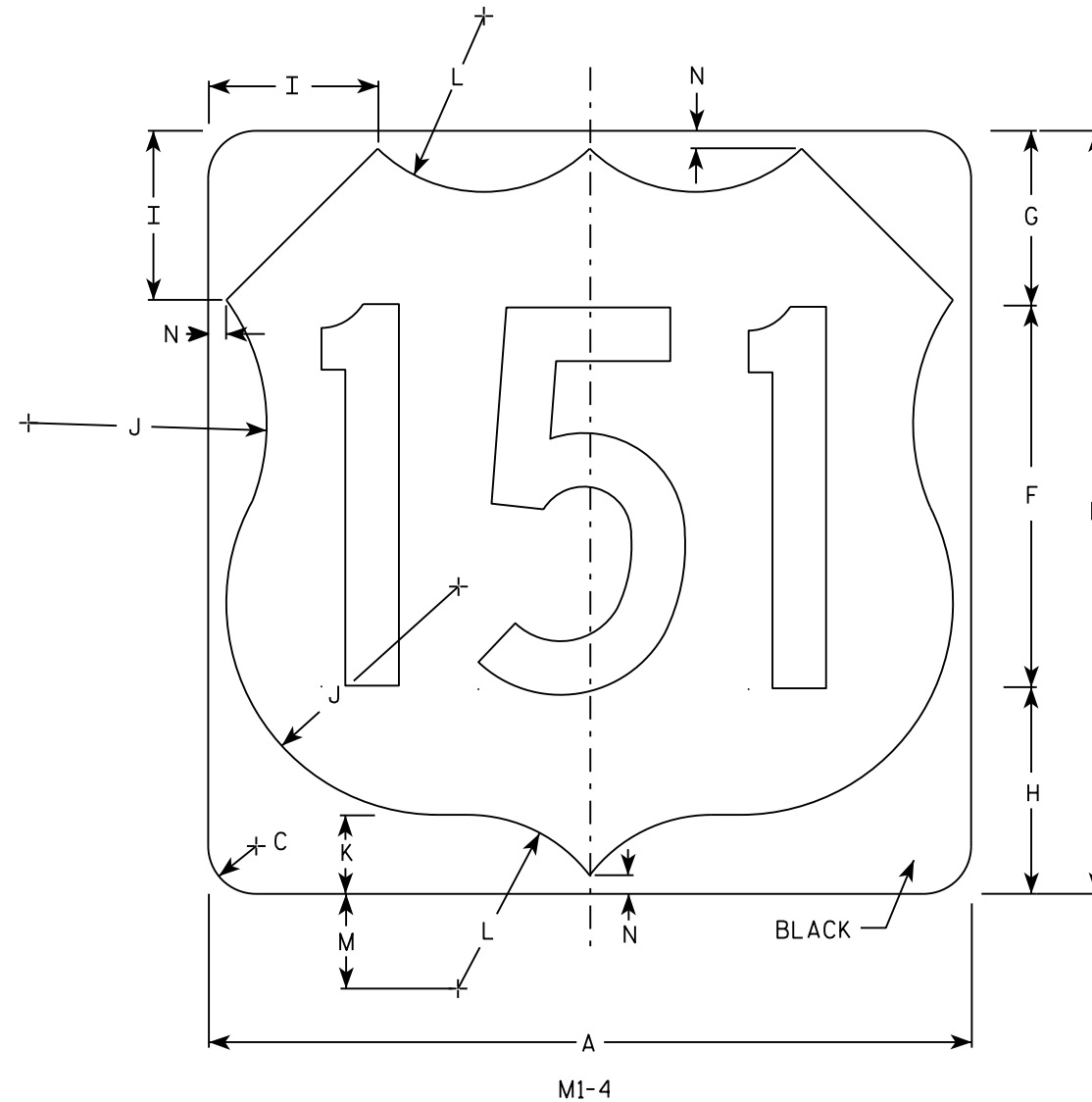
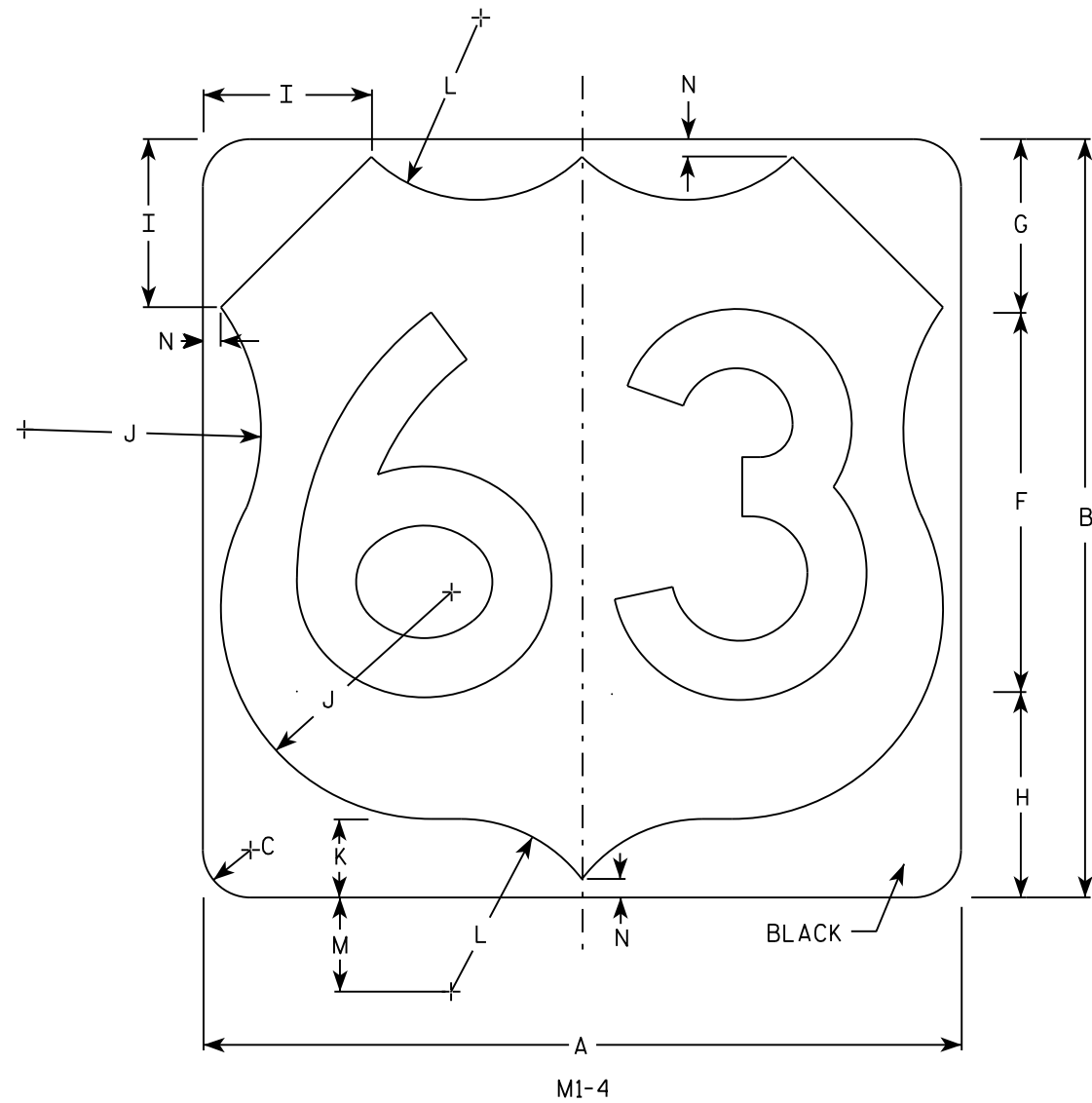
DATE 9/25/19

PLATE NO. G20-57C.1

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - D except 3 number signs Series C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	24	1 1/2			12	5 1/2	6 1/2	5	7 1/2	2 1/2	5 1/2	3	1/2													4.0
3	36	36	2 1/4			18	8 1/4	9 1/4	7 1/4	11 1/4	3 3/4	8 1/4	4 1/2	3/4													9.0
4	36	36	2 1/4			18	8 1/4	9 1/4	7 1/4	11 1/4	3 3/4	8 1/4	4 1/2	3/4													9.0
5	36	36	2 1/4			18	8 1/4	9 1/4	7 1/4	11 1/4	3 3/4	8 1/4	4 1/2	3/4													9.0

USH MARKER
M1-4 FOR ASSEMBLIES

WISCONSIN DEPT OF TRANSPORTATION

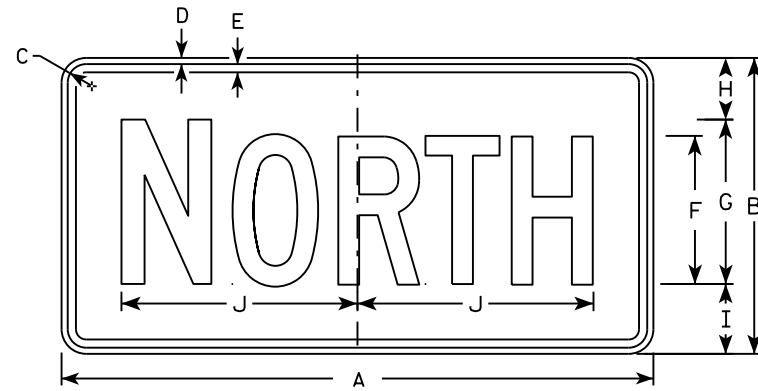
APPROVED *Matthew R Rauch*
For State Traffic Engineer

DATE 3/16/18 PLATE NO. M1-4.10

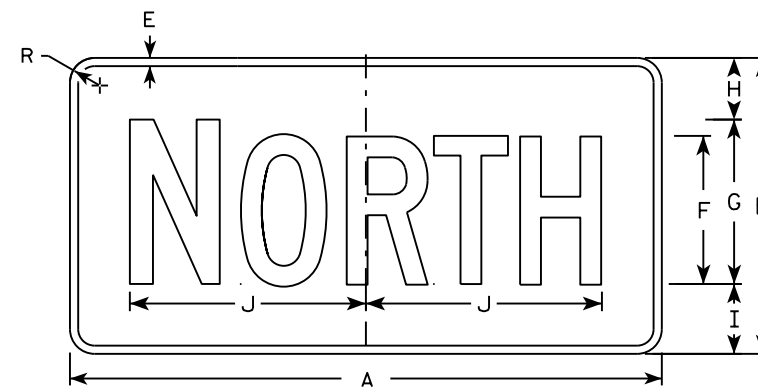
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ **E**

NOTES

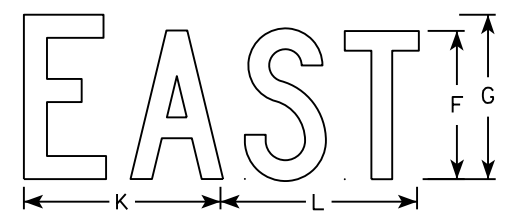
- All Signs Type II - Type H
- Color:
 - Background - See note 5
 - Message - See note 5
- Message Series - C
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- M3-1 thru M3-4 Background - White
 Message - Black
 MB3-1 thru MB3-4 Background - Blue
 Message - White
 MK3-1 thru MK3-4 Background - Green
 Message - White
 MM3-1 thru MM3-4 Background - White
 Message - Green
 MN3-1 thru MN3-4 Background - Brown
 Message - White
 MP3-1 thru MP3-4 Background - White
 Message - Blue
- Note the first letter of each direction is larger than the remainder of the message.



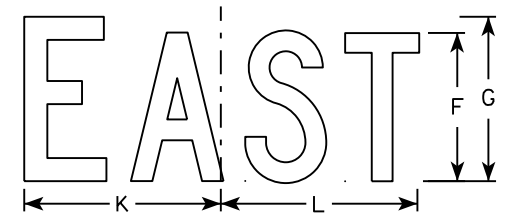
M3-1
MM3-1
MP3-1



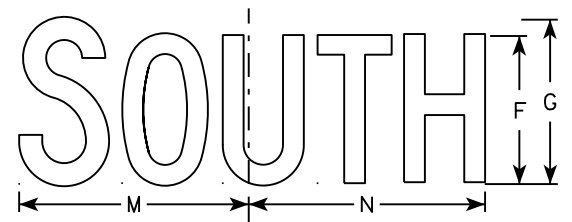
MB3-1
MK3-1
MN3-1



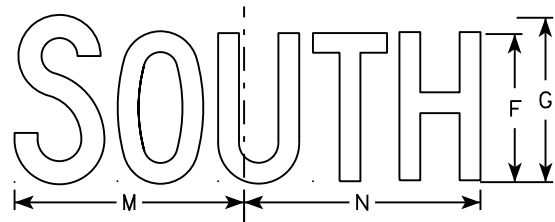
M3-2
MM3-2
MP3-2



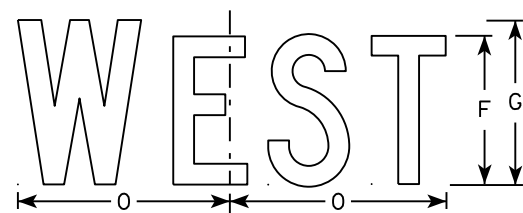
MB3-2
MK3-2
MN3-2



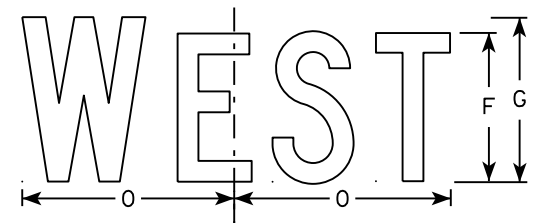
M3-3
MM3-3
MP3-3



MB3-3
MK3-3
MN3-3



M3-4
MM3-4
MP3-4



MB3-4
MK3-4
MN3-4

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	12	1 1/8	3/8	3/8	6	7	2 1/4	2 3/4	10 1/4	7 7/8	8 3/8	10 1/4	9 3/4	8 3/4			1 1/2									2.00
3	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
4	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5
5	36	18	1 1/8	3/8	1/2	9	10	3 3/4	4 1/4	14 3/8	12	12 1/8	14	14 1/8	13			1 1/2									4.5

STANDARD SIGNS
M3-1 thru M3-4
SERIES

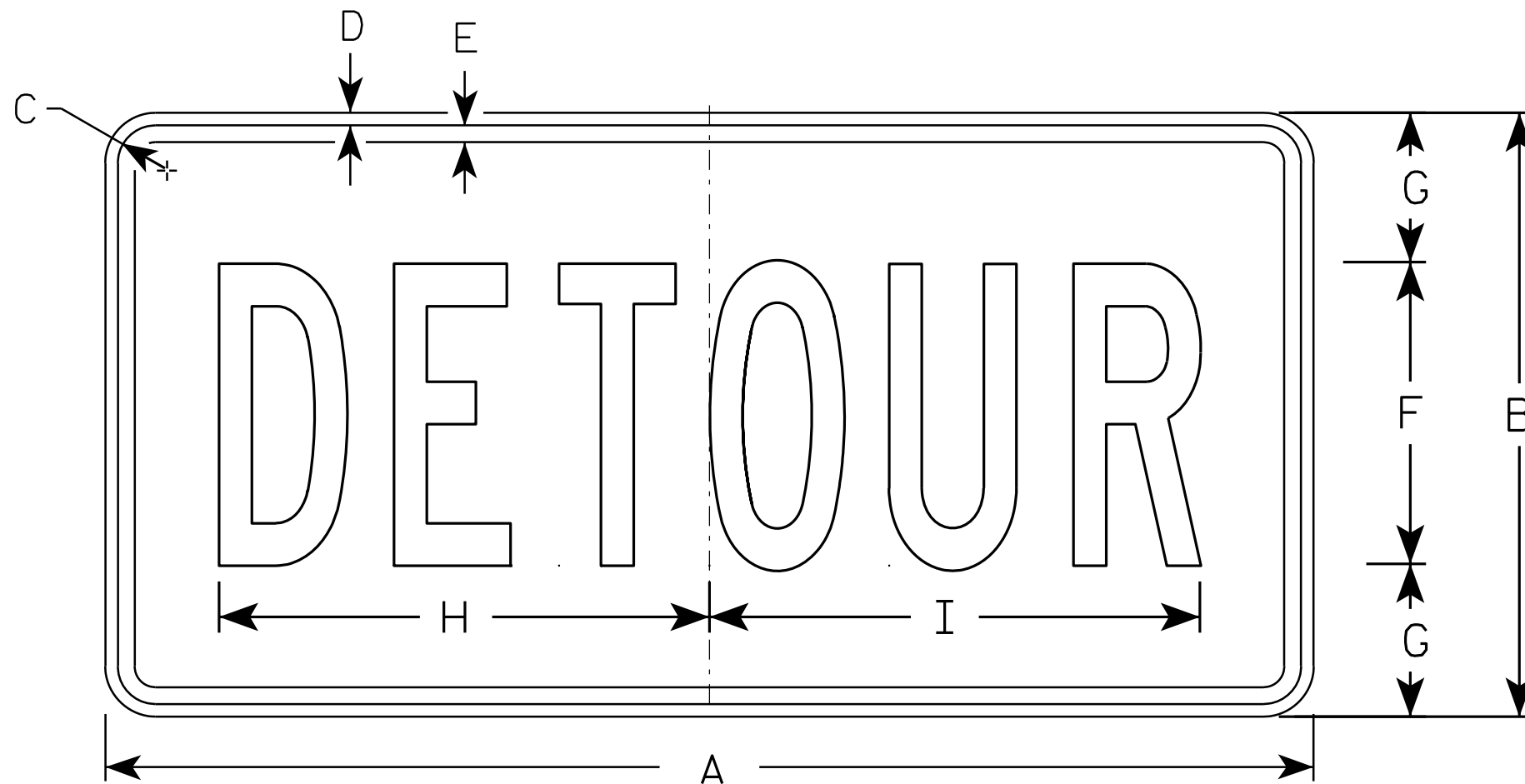
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 10/15/15 PLATE NO. M3-1.14

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - B
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



M4-8

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	12	1 1/8	3/8	3/8	6	3	10	10 1/4																		2.0
3	36	18	1 1/8	3/8	1/2	9	4 1/2	14 5/8	14 1/2																		4.5
4																											
5																											

STANDARD SIGN
M4-8

WISCONSIN DEPT OF TRANSPORTATION

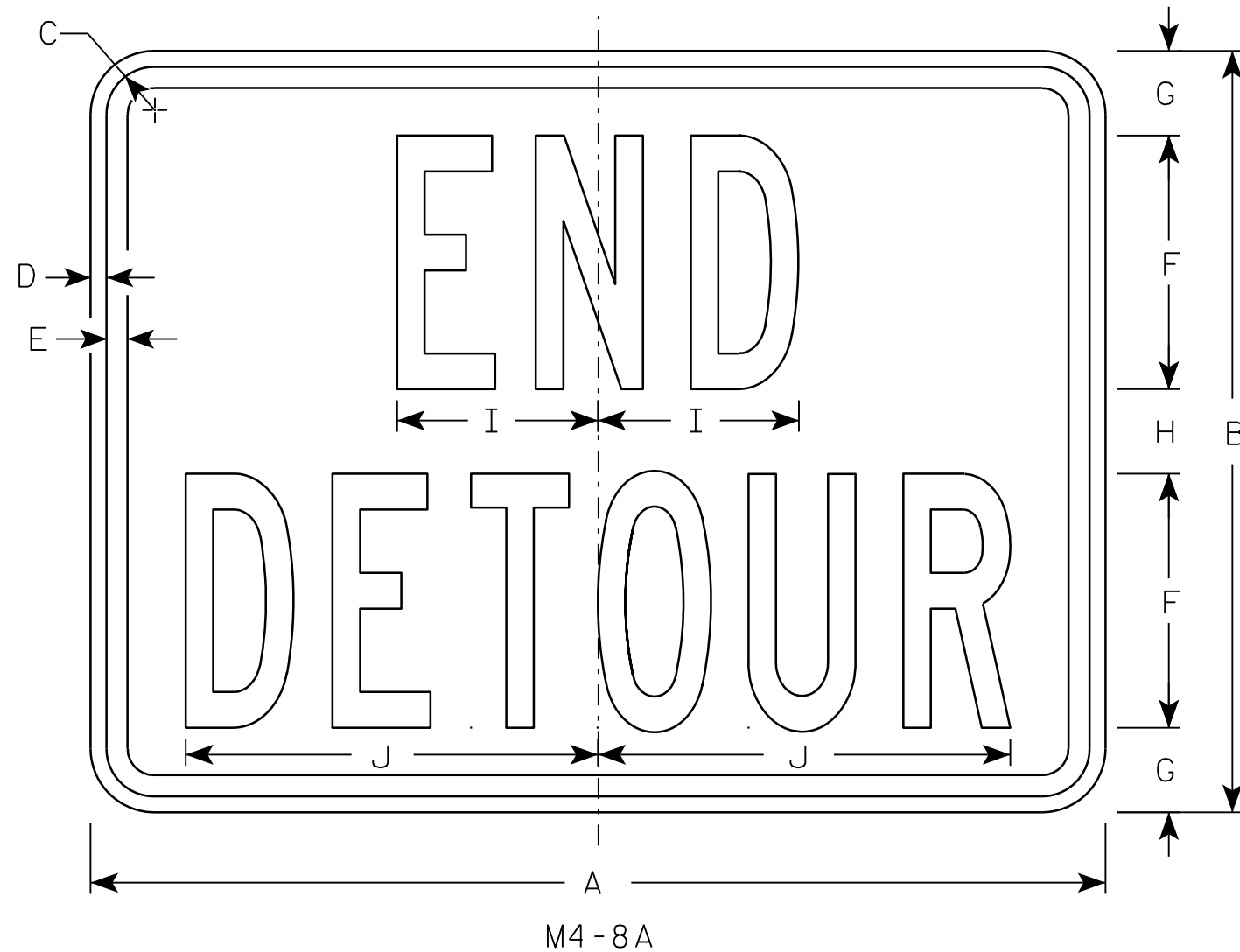
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 11/10/10 PLATE NO. M4-8.2

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - B
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	24	18	1 1/8	3/8	1/2	6	2	2	4 3/4	9 3/4																	3.0
3	30	24	1 1/8	3/8	1/2	8	2 1/2	3	6 3/4	13																	5.0
4																											
5																											

STANDARD SIGN
M4-8A

WISCONSIN DEPT OF TRANSPORTATION

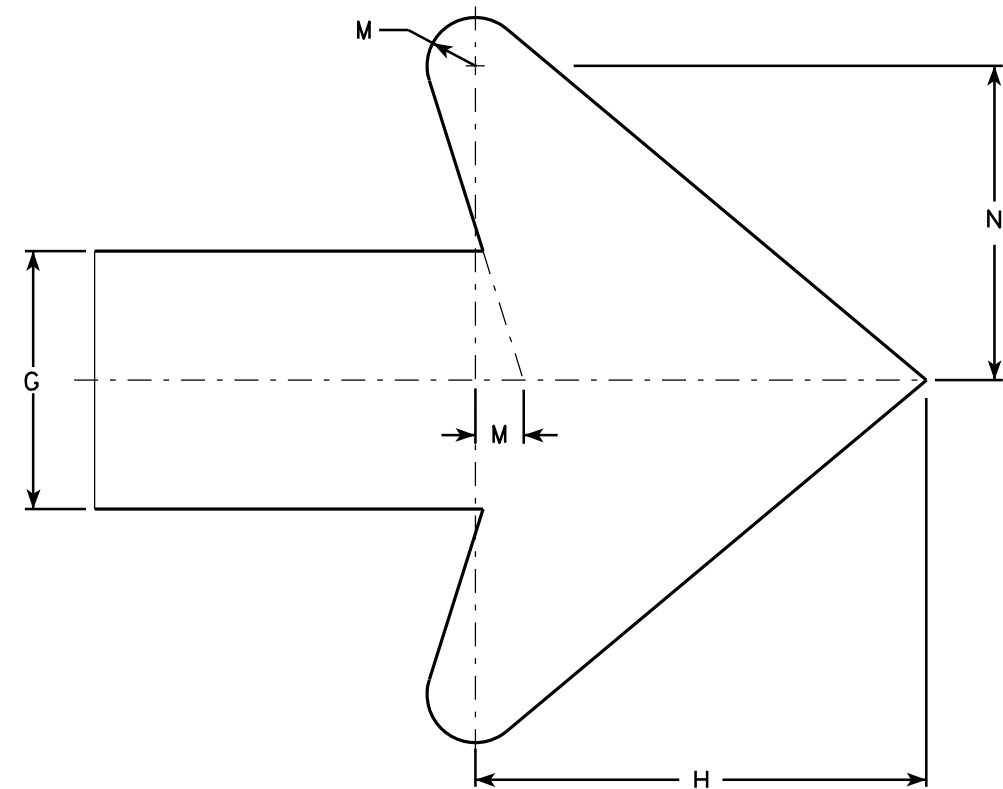
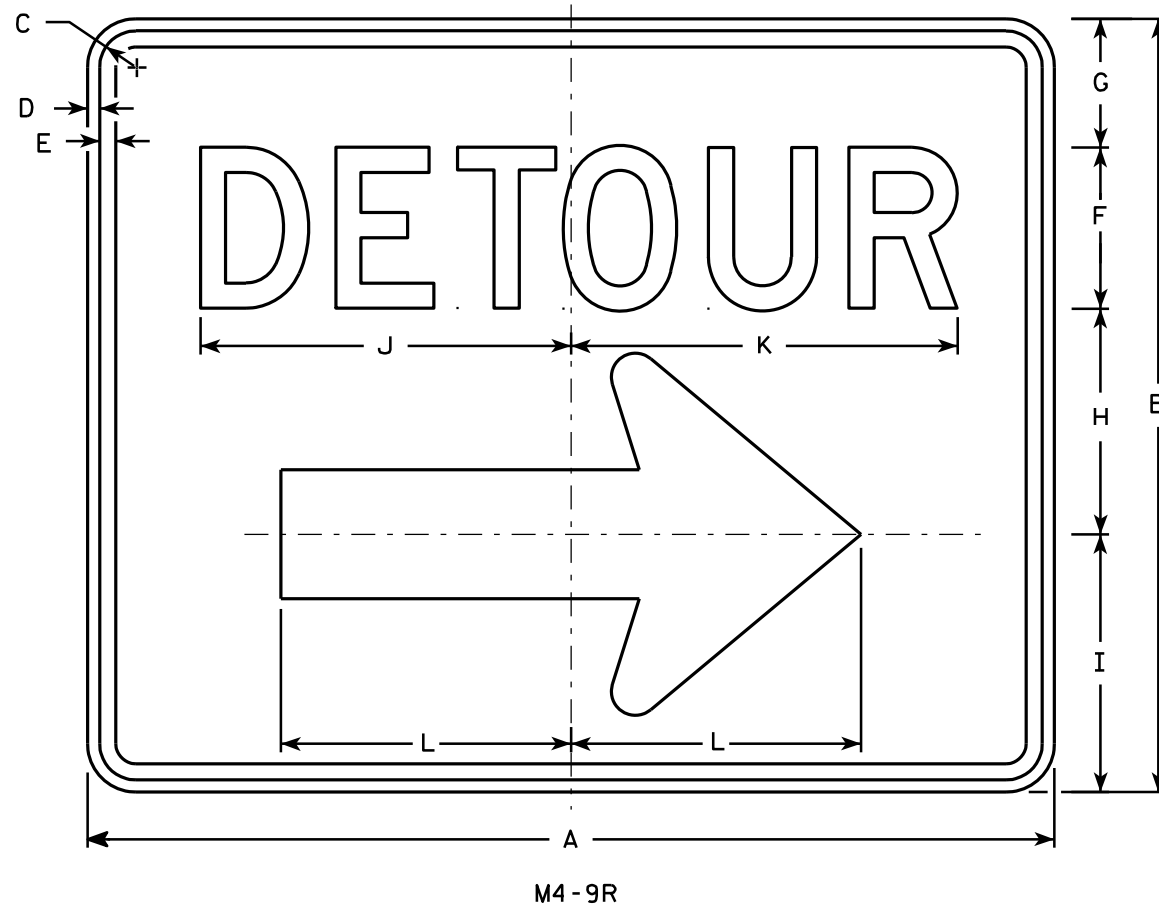
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 3/9/11 PLATE NO. M4-8A.2

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. M4-9L is the same as M4-9R except the arrow is reversed.



SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	30	24	1 1/8	3/8	1/2	5	4	7	8	11 1/2	12	9	3/4	4 7/8													5.00
3	30	24	1 1/8	3/8	1/2	5	4	7	8	11 1/2	12	9	3/4	4 7/8													5.00
4	48	36	1 3/8	1/2	5/8	8	6	10 1/2	11 5/8	20 5/8	20 1/2	13 1/4	1 1/8	6 7/8													12.0
5	48	36	1 3/8	1/2	5/8	8	6	10 1/2	11 5/8	20 5/8	20 1/2	13 1/4	1 1/8	6 7/8													12.0

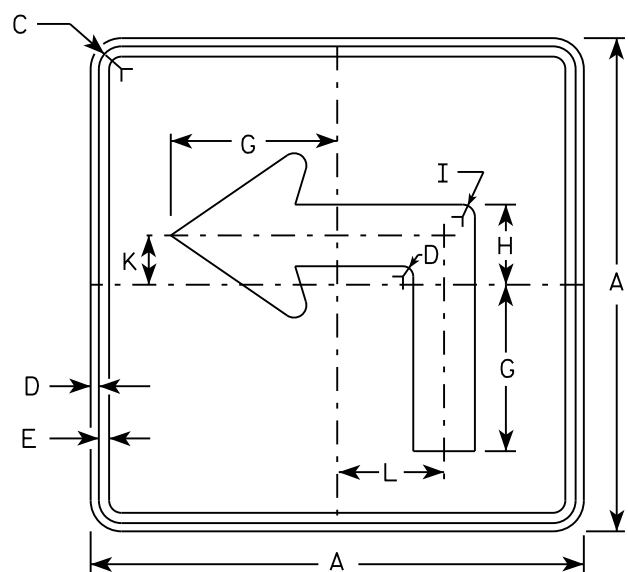
STANDARD SIGN
M4-9 R & L

WISCONSIN DEPT OF TRANSPORTATION

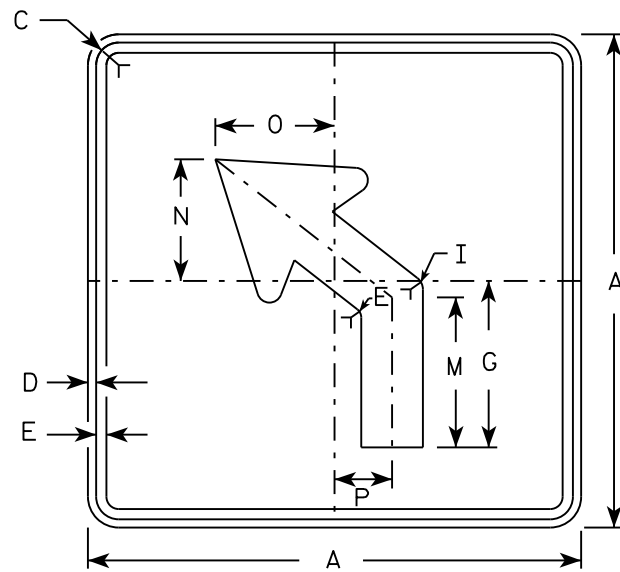
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 3/9/11 PLATE NO. M4-9R.4

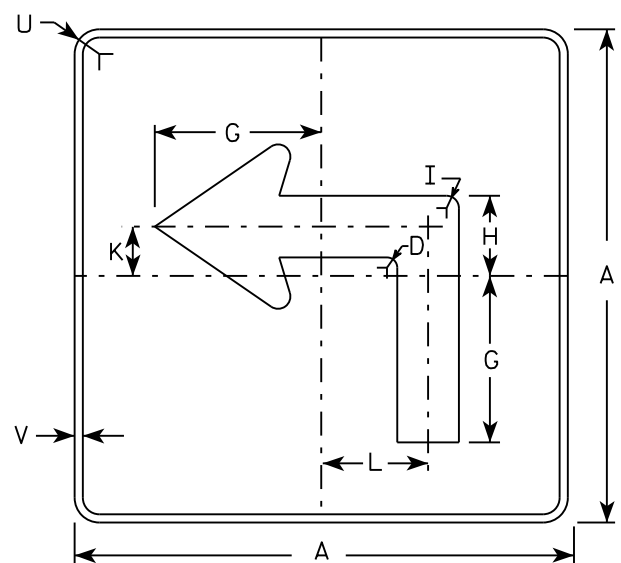
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**



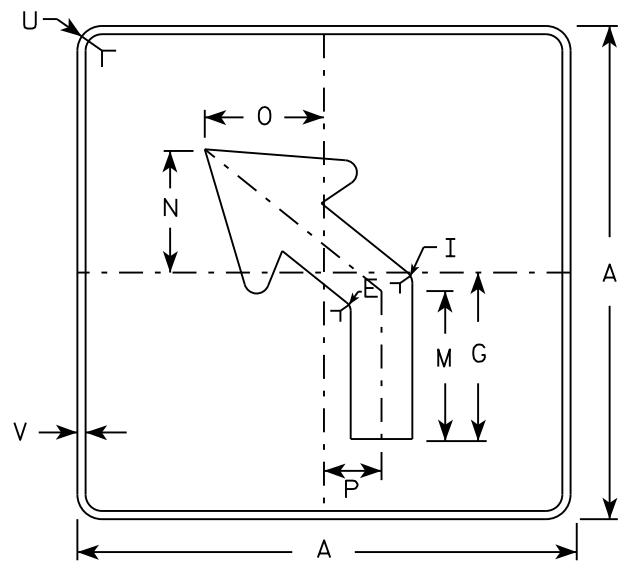
M5-1L
MM5-1L
M05-1L
MP5-1L



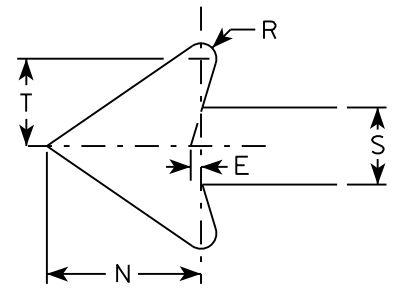
M5-2L
MM5-2L
M05-2L
MP5-2L



MB5-1L
MK5-1L
MN5-1L
MR5-1L



MB5-2L
MK5-2L
MN5-2L
MR5-2L



NOTES

- Signs are Type II - Type H reflective except as shown
- Color:
 - Background - See note 4
 - Message - See note 4
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- | | |
|-----------------|---|
| M5-1 and M5-2 | Background - White |
| | Message - Black |
| MB5-1 and MB5-2 | Background - Blue |
| | Message - White |
| MK5-1 and MK5-2 | Background - Green |
| | Message - White |
| MM5-1 and MM5-2 | Background - White |
| | Message - Green |
| MN5-1 and MN5-2 | Background - Brown |
| | Message - White |
| M05-1 and M05-2 | Background - Orange - Type F Reflective |
| | Message - Black |
| MP5-1 and MP5-2 | Background - White - Type H Reflective |
| | Message - Blue |
| MR5-1 and MR5-2 | Background - Brown |
| | Message - Yellow |
- M5-1R same as M5-1L except arrow points right.
- M5-2R same as M5-2L except arrow tilts right.

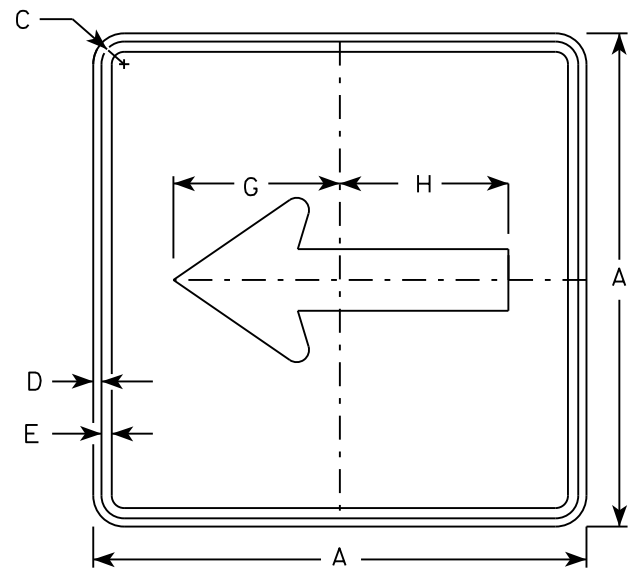
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7	3 3/8	5/8		2 1/8	4 1/2	6 3/8	5 1/4	5	2 1/2		1/2	2 5/8	3	1 1/2	1/2					3.06
3	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25
4	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25
5	30		1 3/8	1/2	5/8		10 1/8	4 7/8	7/8		3	6 1/2	9 1/8	7 1/2	7 1/4	3 1/2		3/4	3 3/4	4 1/4	1 7/8	1/2					6.25

STANDARD SIGN
M5-1 & M5-2

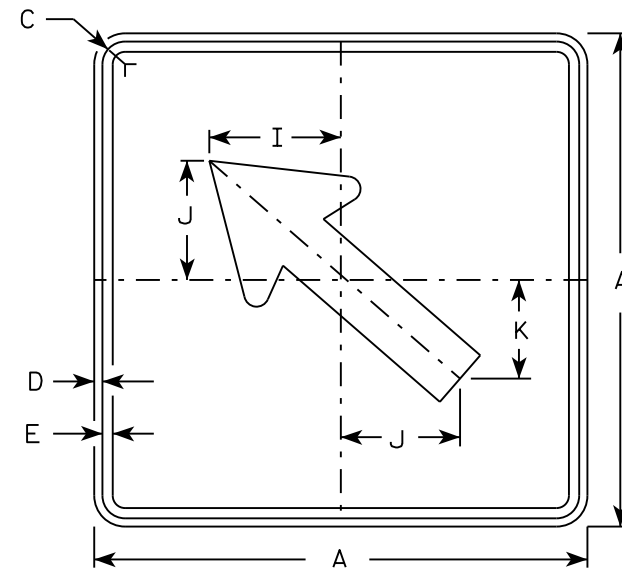
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

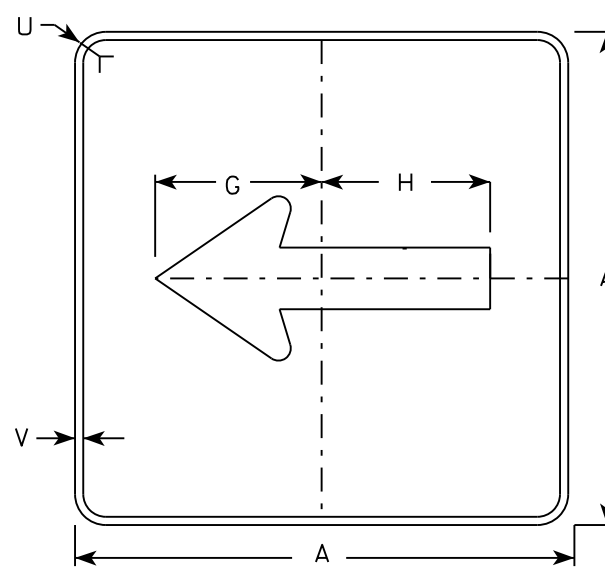
DATE 10/15/15 PLATE NO. M5-1.13



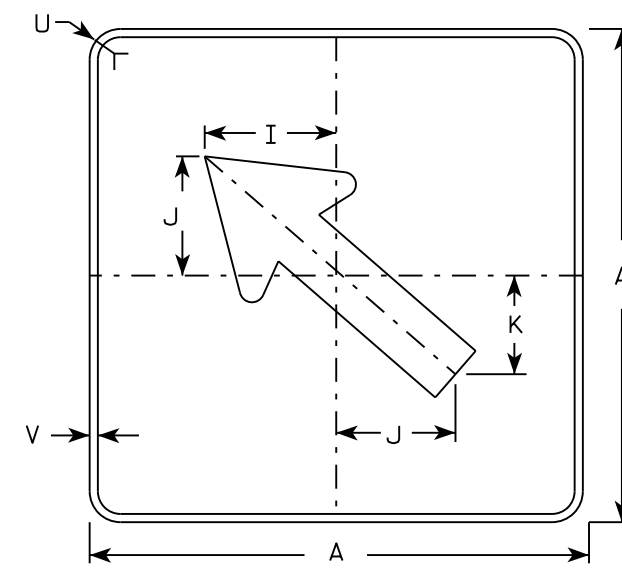
M6-1
MM6-1
M06-1
MP6-1



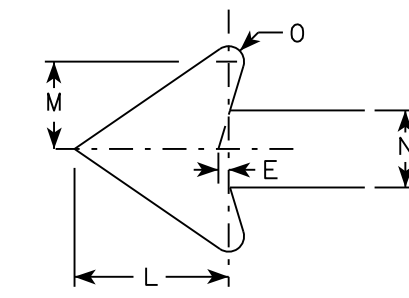
M6-2
MM6-2
M06-2
MP6-2



MB6-1
MK6-1
MN6-1
MR6-1



MB6-2
MK6-2
MN6-2
MR6-2



NOTES

- Signs are Type II - Type H except as Shown
- Color:
Background - See note 4
Message - See note 4
- Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
- M6-1 and M6-2 Background - White
Message - Black
MB6-1 and MB6-2 Background - Blue
Message - White
MK6-1 and MK6-2 Background - Green
Message - White
MM6-1 and MM6-2 Background - White
Message - Green
MN6-1 and MN6-2 Background - Brown
Message - White
M06-1 and M06-2 Background - Orange - Type F Reflective
Message - Black
MP6-1 and MP6-2 Background - White
Message - Blue
MR6-1 and MR6-2 Background - Brown
Message - Yellow

7

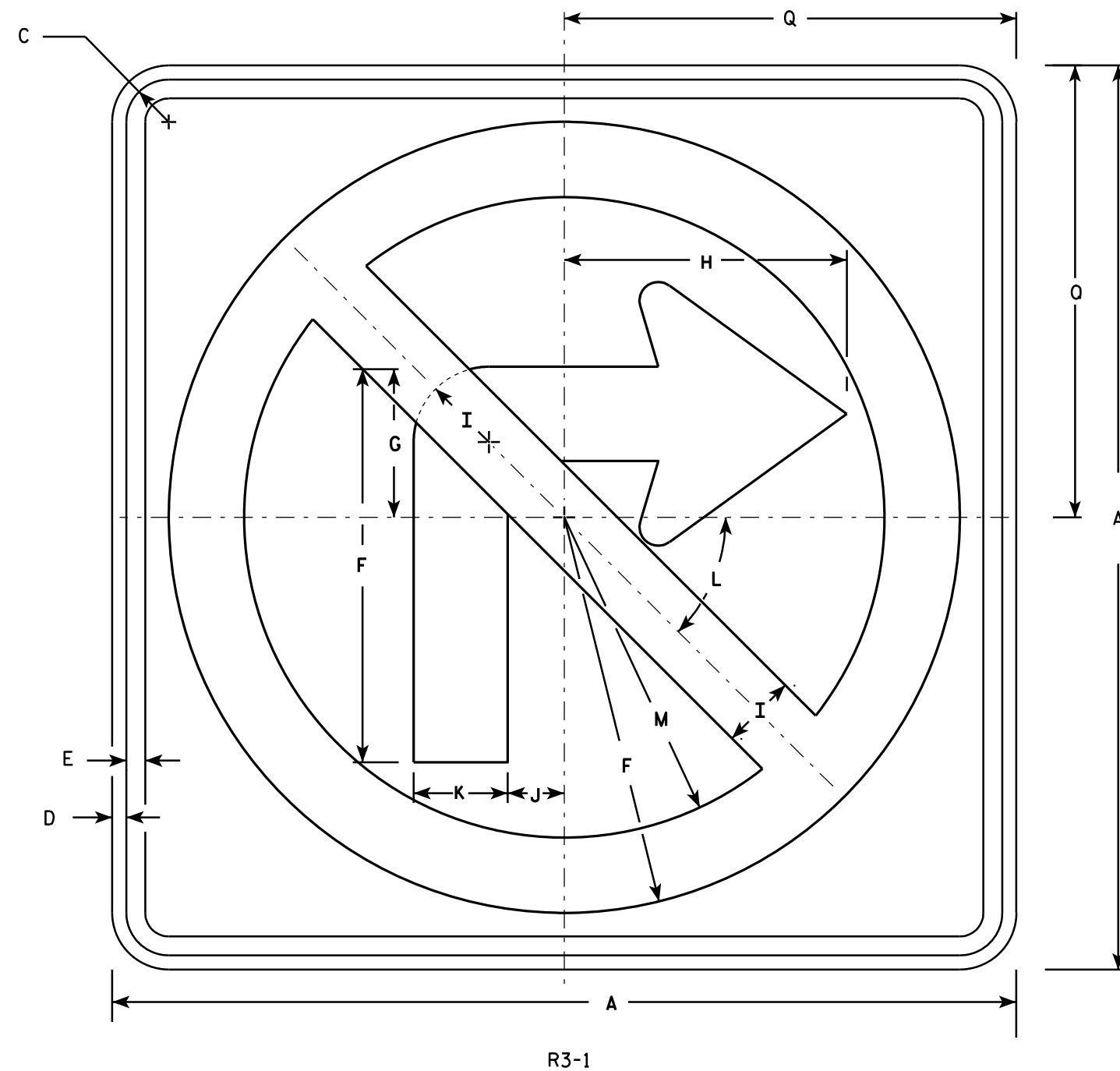
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2	21		1 1/8	3/8	3/8		7 1/2	7 1/8	5 5/8	5	4 1/4	5 1/4	3	2 5/8	1/2						1 1/2	1/2					3.06
3	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25
4	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25
5	30		1 3/8	1/2	5/8		10 3/4	10 1/4	8	7 1/4	6	7 1/2	4 1/4	3 3/4	3/4						1 7/8	1/2					6.25

STANDARD SIGN
M6-1 & M6-2
SERIES

WISCONSIN DEPT OF TRANSPORTATION

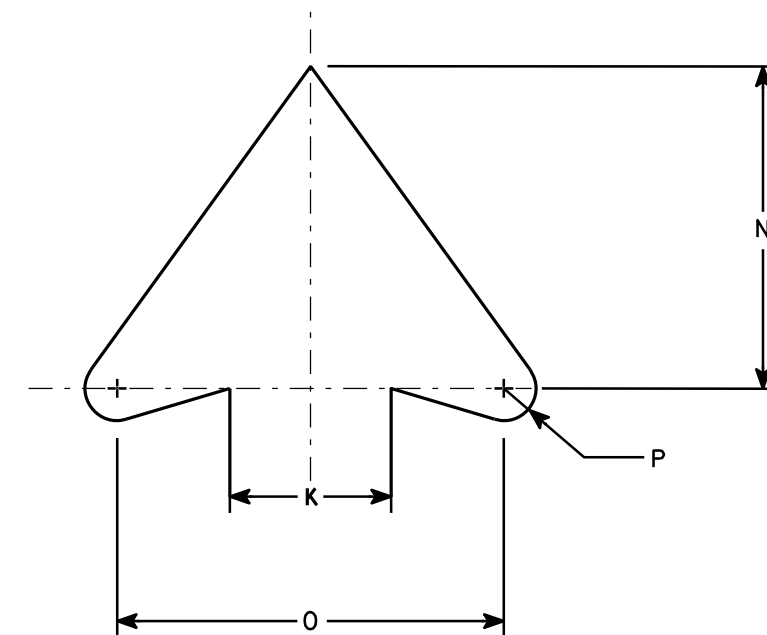
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 10/15/15 PLATE NO. M6-1.15



NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - See note 4
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. Border & Arrow are non reflective black, the circle with diagonal bar is reflective red.



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	24		1 1/8	3/8	1/2	10 1/2	4	7 1/2	2	1 1/2	2 1/2	45	8 1/2	5	6	1/2	12										4.0
2S	24		1 1/8	3/8	1/2	10 1/2	4	7 1/2	2	1 1/2	2 1/2	45°	8 1/2	5	6	1/2	12										4.0
2M	36		1 5/8	5/8	3/4	15 3/4	6	11 1/4	3	2 1/4	3 3/4	45	12 3/4	7 1/2	9	3/4	18										9.0
3	36		1 5/8	5/8	3/4	15 3/4	6	11 1/4	3	2 1/4	3 3/4	45	12 3/4	7 1/2	9	3/4	18										9.0
4	36		1 5/8	5/8	3/4	15 3/4	6	11 1/4	3	2 1/4	3 3/4	45°	12 3/4	7 1/2	9	3/4	18										9.0
5	48		2 1/4	3/4	1	21	8	15	4	3	5	45°	17	10	12	1	24										16.0

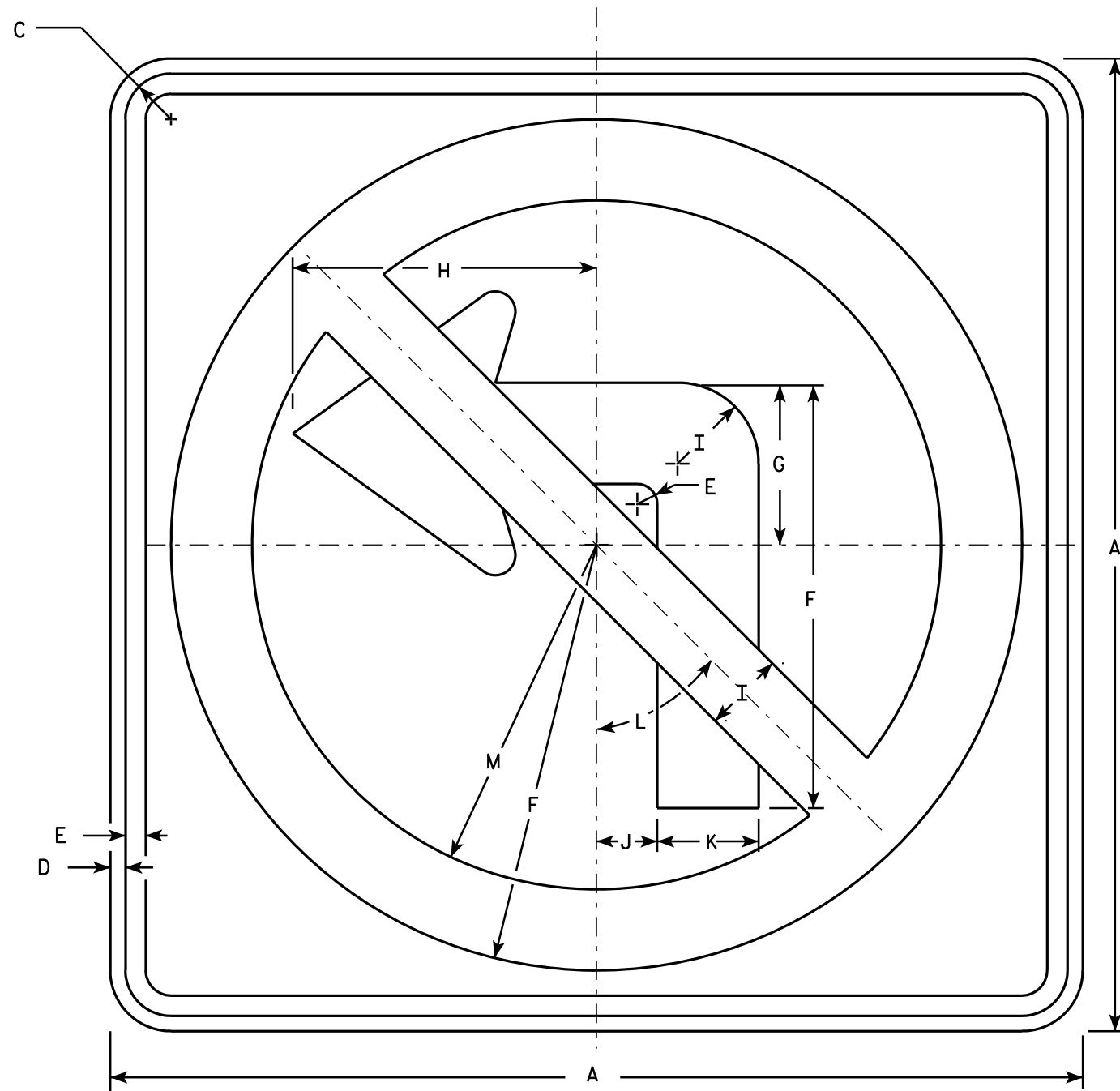
STANDARD SIGN
R3-1

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 12/08/10 PLATE NO. R3-1.5

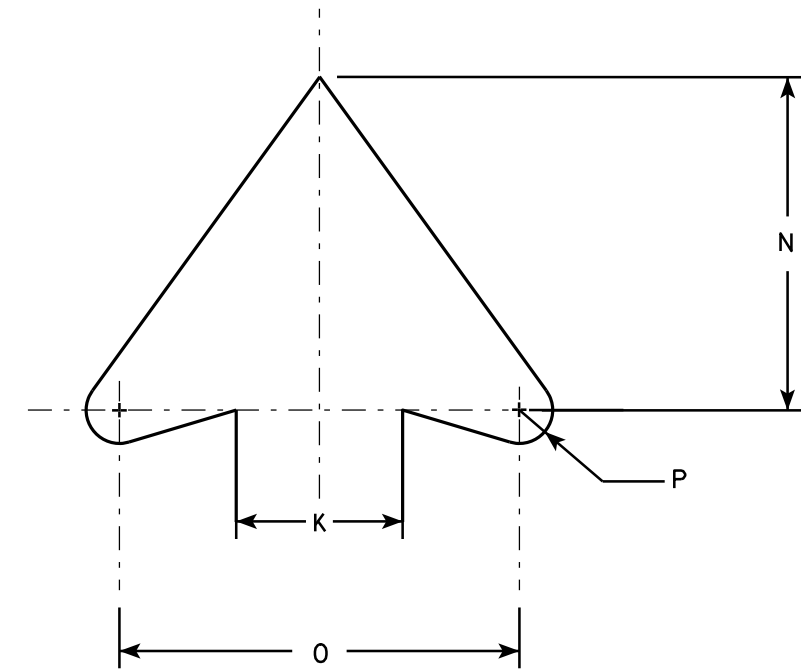
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E



R3-2

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - See note 4
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
4. Border & Arrow are non reflective black, the circle with diagonal bar is reflective red.



ARROW DETAIL

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. Ft.
1	24		1 1/8	3/8	1/2	10 1/2	4	7 1/2	2	1 1/2	2 1/2	45°	8 1/2	5	6	1/2											4.0
2S	24		1 1/8	3/8	1/2	10 1/2	4	7 1/2	2	1 1/2	2 1/2	45°	8 1/2	5	6	1/2											4.0
2M	36		1 5/8	5/8	3/4	15 3/4	6	11 1/4	3	2 1/4	3 3/4	45°	12 3/4	7 1/2	9	3/4											9.0
3	36		1 5/8	5/8	3/4	15 3/4	6	11 1/4	3	2 1/4	3 3/4	45°	12 3/4	7 1/2	9	3/4											9.0
4	36		1 5/8	5/8	3/4	15 3/4	6	11 1/4	3	2 1/4	3 3/4	45°	12 3/4	7 1/2	9	3/4											9.0
5	48		2 1/4	3/4	1	21	8	15	4	3	5	45°	17	10	12	1											16.0

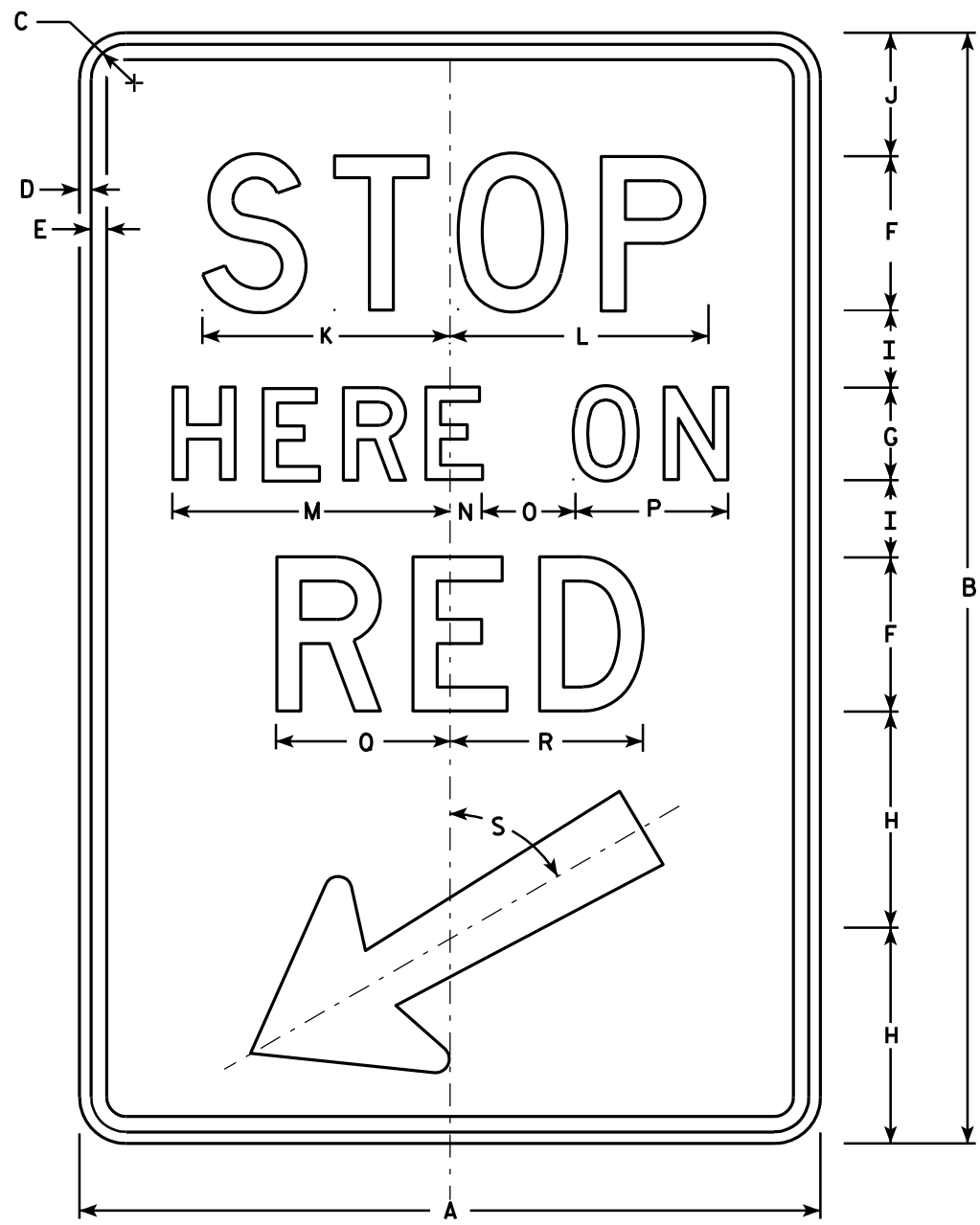
STANDARD SIGN
R3-2

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 12/08/10 PLATE NO. R3-2.10

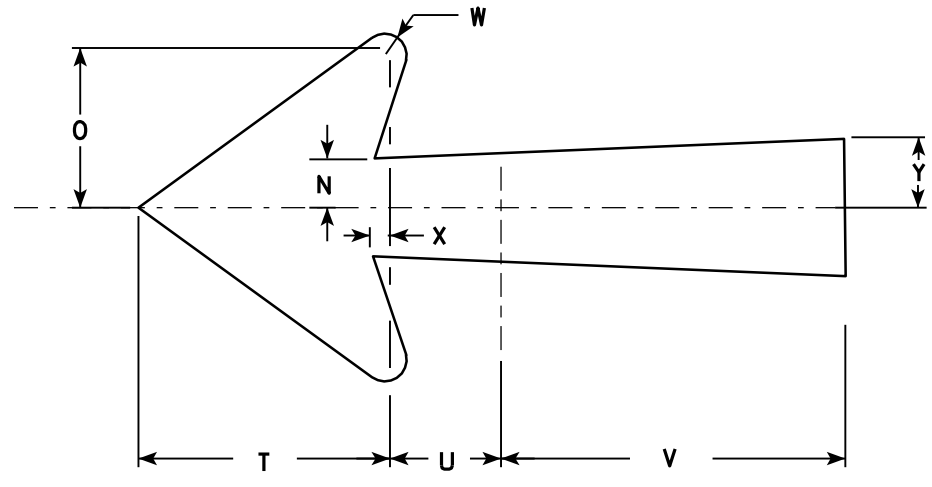
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**



R10-6

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



Arrow Detail

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	24	36	1 1/8	3/8	1/2	5	3	7	2 1/2	4	8	8 3/8	9	1	3	5	5 5/8	6 1/4	60°	5 1/4	2 1/4	7 1/8	1/2	3/8	1 3/8		6.0
2M	24	36	1 1/8	3/8	1/2	5	3	7	2 1/2	4	8	8 3/8	9	1	3	5	5 5/8	6 1/4	60°	5 1/4	2 1/4	7 1/8	1/2	3/8	1 3/8		6.0
3																											
4																											
5																											

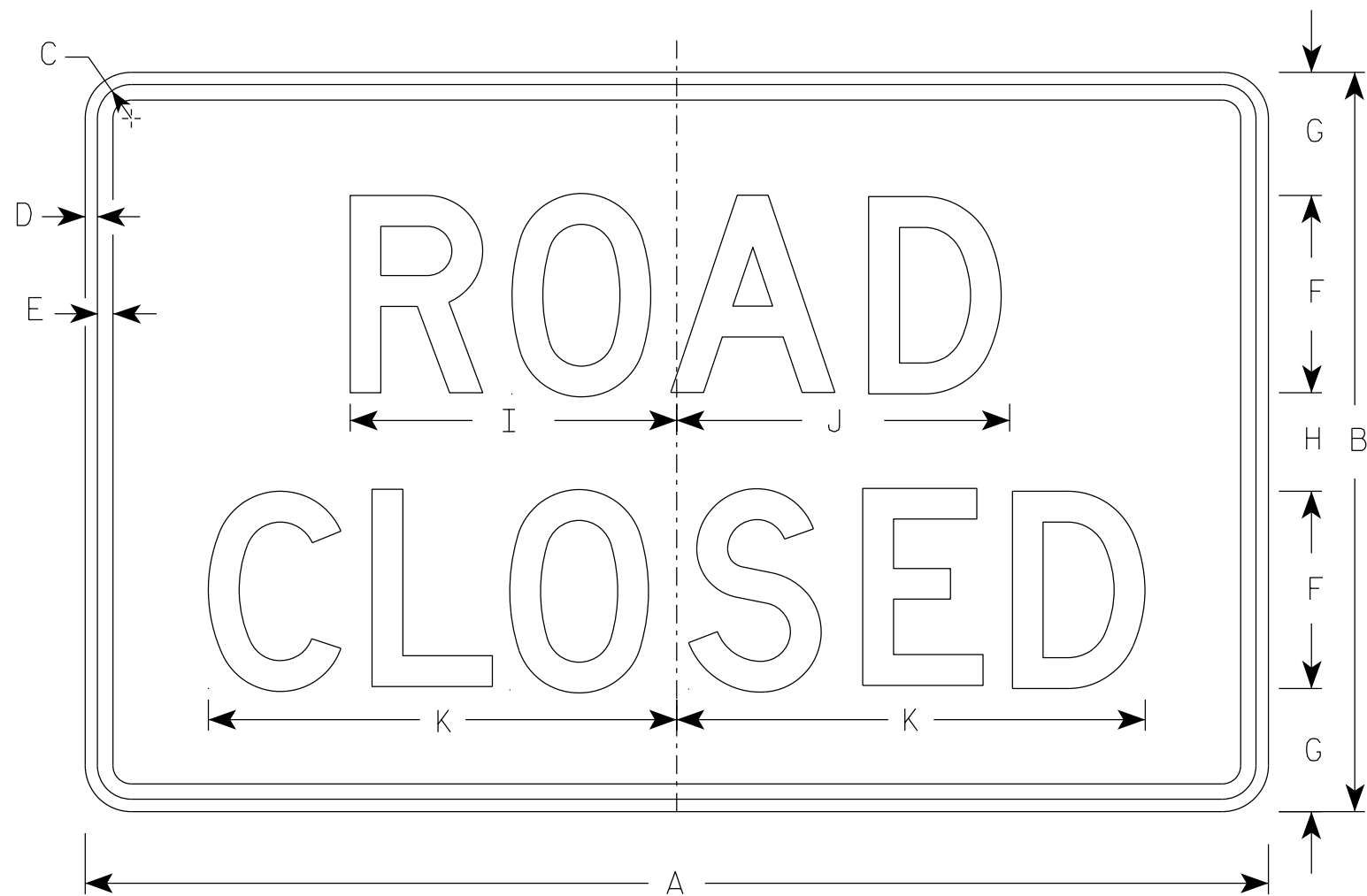
STANDARD SIGN
R10-6

WISCONSIN DEPT OF TRANSPORTATION

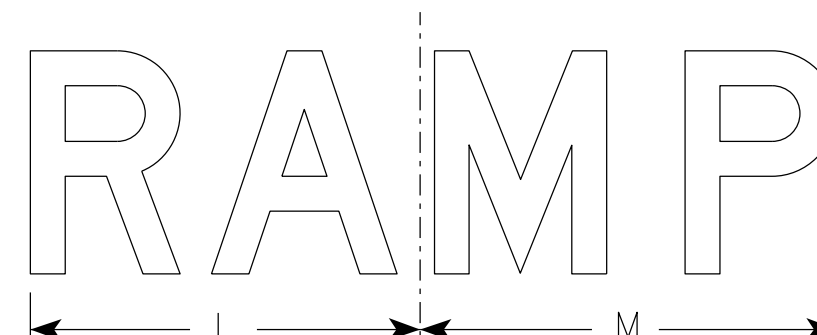
APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 4/5/11 PLATE NO. R10-6.6

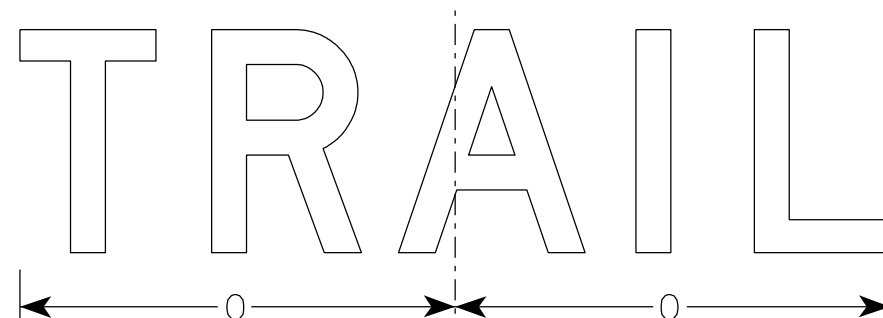
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E



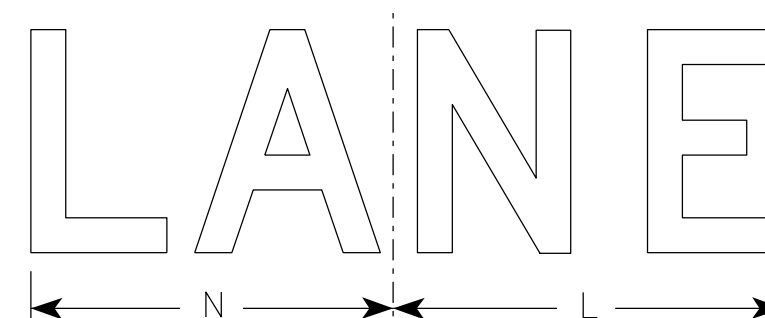
R11-2



R11-2R



R11-2T



R11-2L

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Modify the message as required.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
2M	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
3	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
4	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8												10.0
5	48	30	1 3/8	1/2	5/8	8	5	4	13 1/4	13 1/2	19	14	15	13	15 5/8											10.0	

STANDARD SIGN
R11-2

WISCONSIN DEPT OF TRANSPORTATION

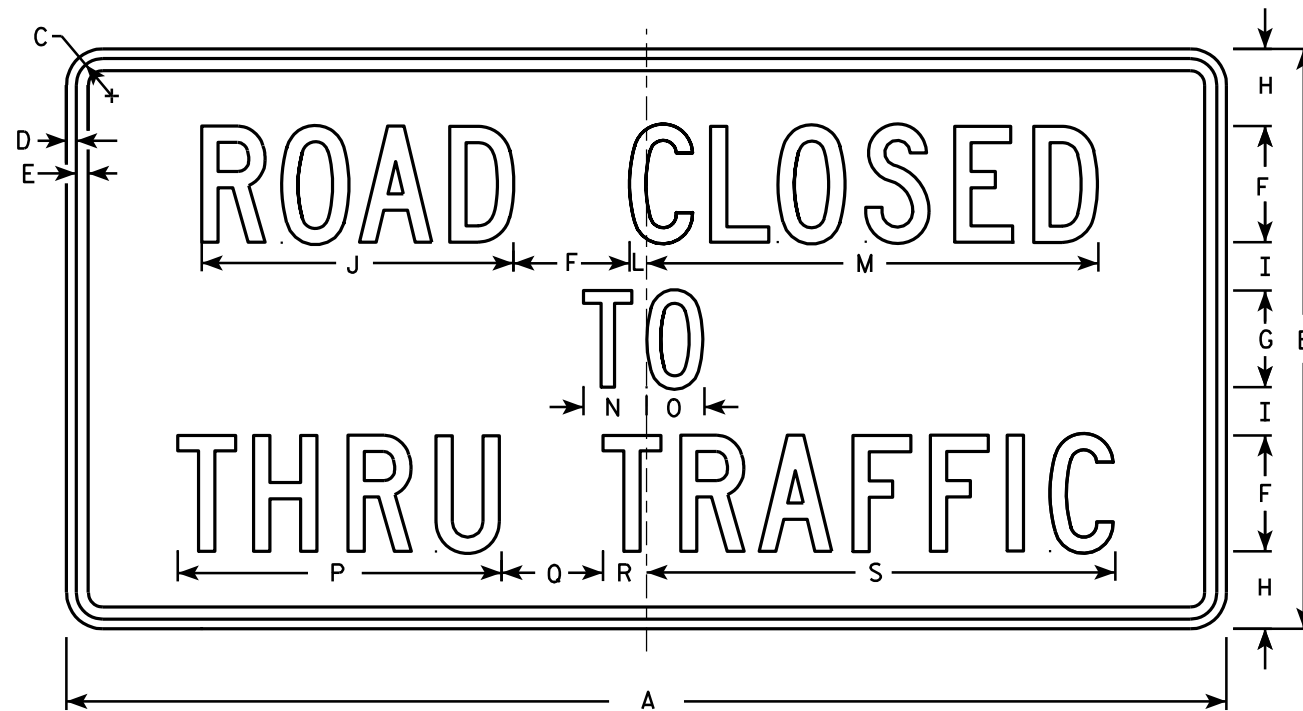
APPROVED *Matthew R Rauch*
For State Traffic Engineer

DATE 3/29/2021 PLATE NO. R11-2.11

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - White
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



R11-4

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	60	30	1 3/8	1/2	5/8	6	5	4	2 1/2	16 1/8		7/8	23 3/8	3 1/4	3	16 3/4	5 1/4	2 1/4	24 1/4								12.5
2M	60	30	1 3/8	1/2	5/8	6	5	4	2 1/2	16 1/8		7/8	23 3/8	3 1/4	3	16 3/4	5 1/4	2 1/4	24 1/4								12.5
3																											
4																											
5																											

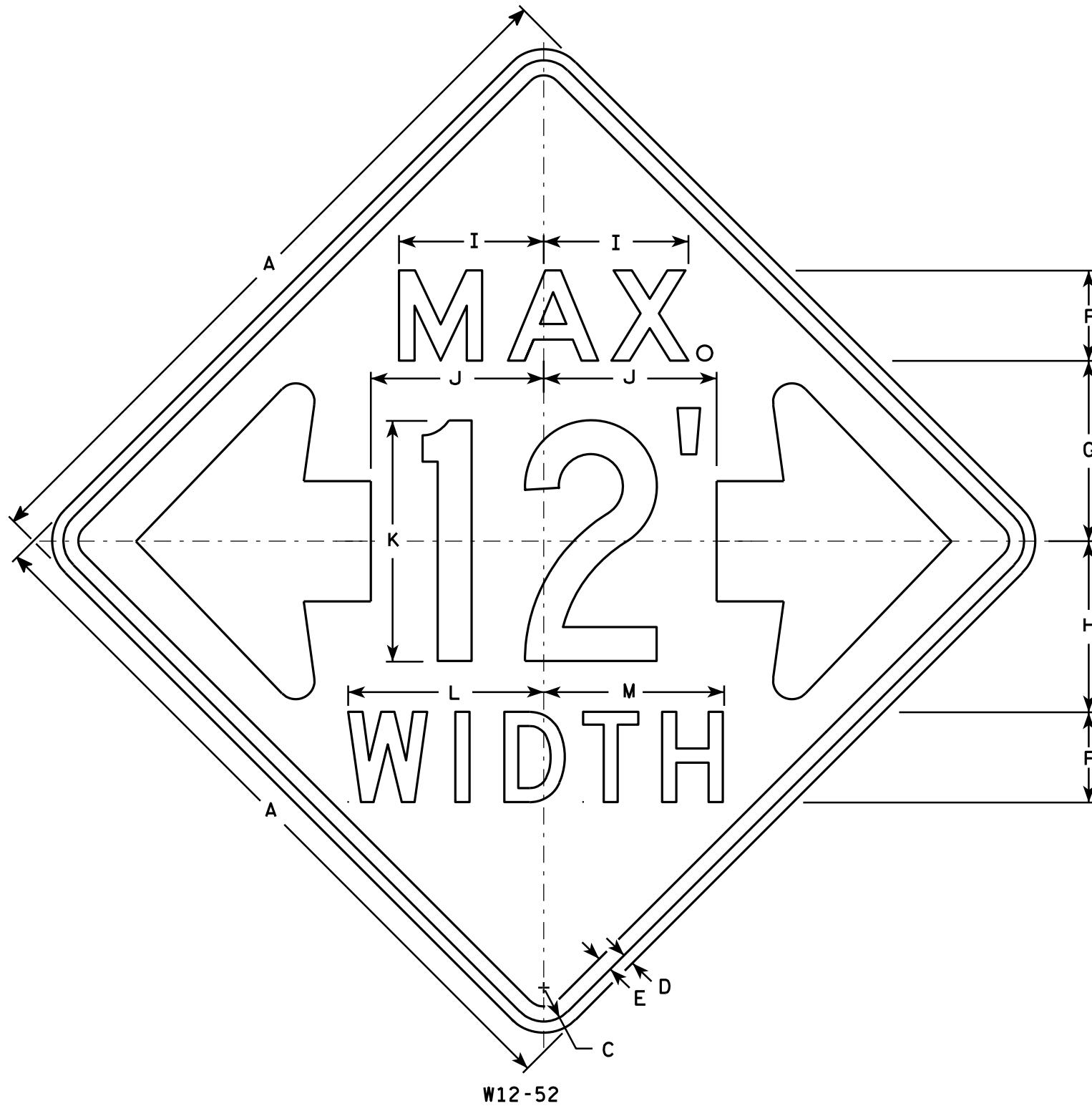
STANDARD SIGN
R11 - 4

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Raush*
for State Traffic Engineer

DATE 4/1/11 PLATE NO. R11-4.3

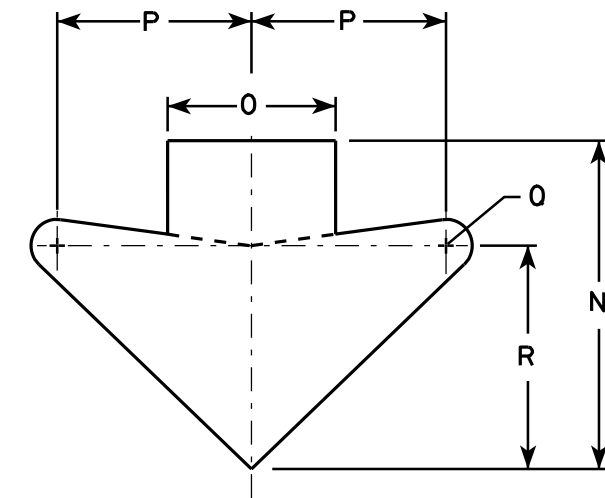
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E



W12-52

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - See note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. The top line is series E, the numerals are series C, and the bottom line is series D.
6. Substitute appropriate numerals and adjust spacing as required.



ARROW DETAIL

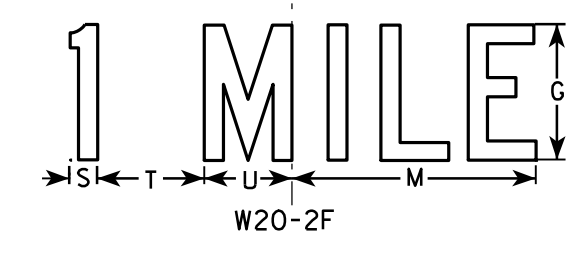
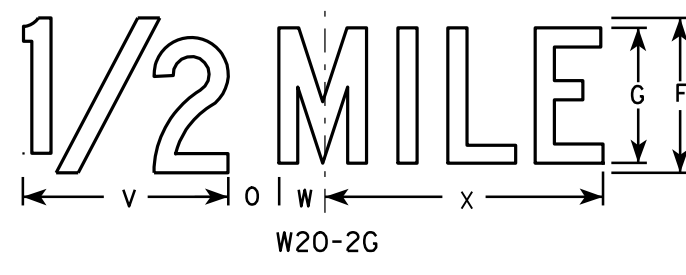
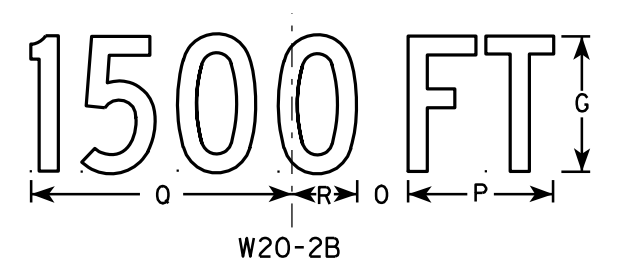
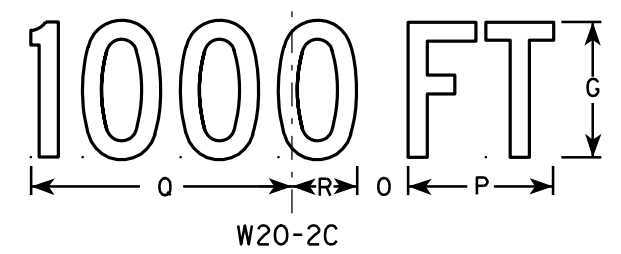
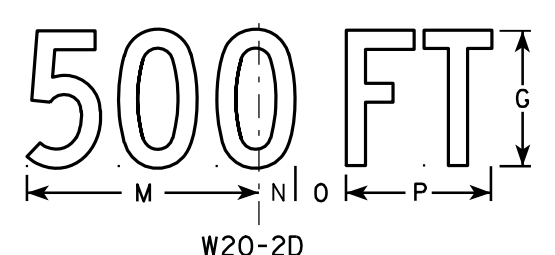
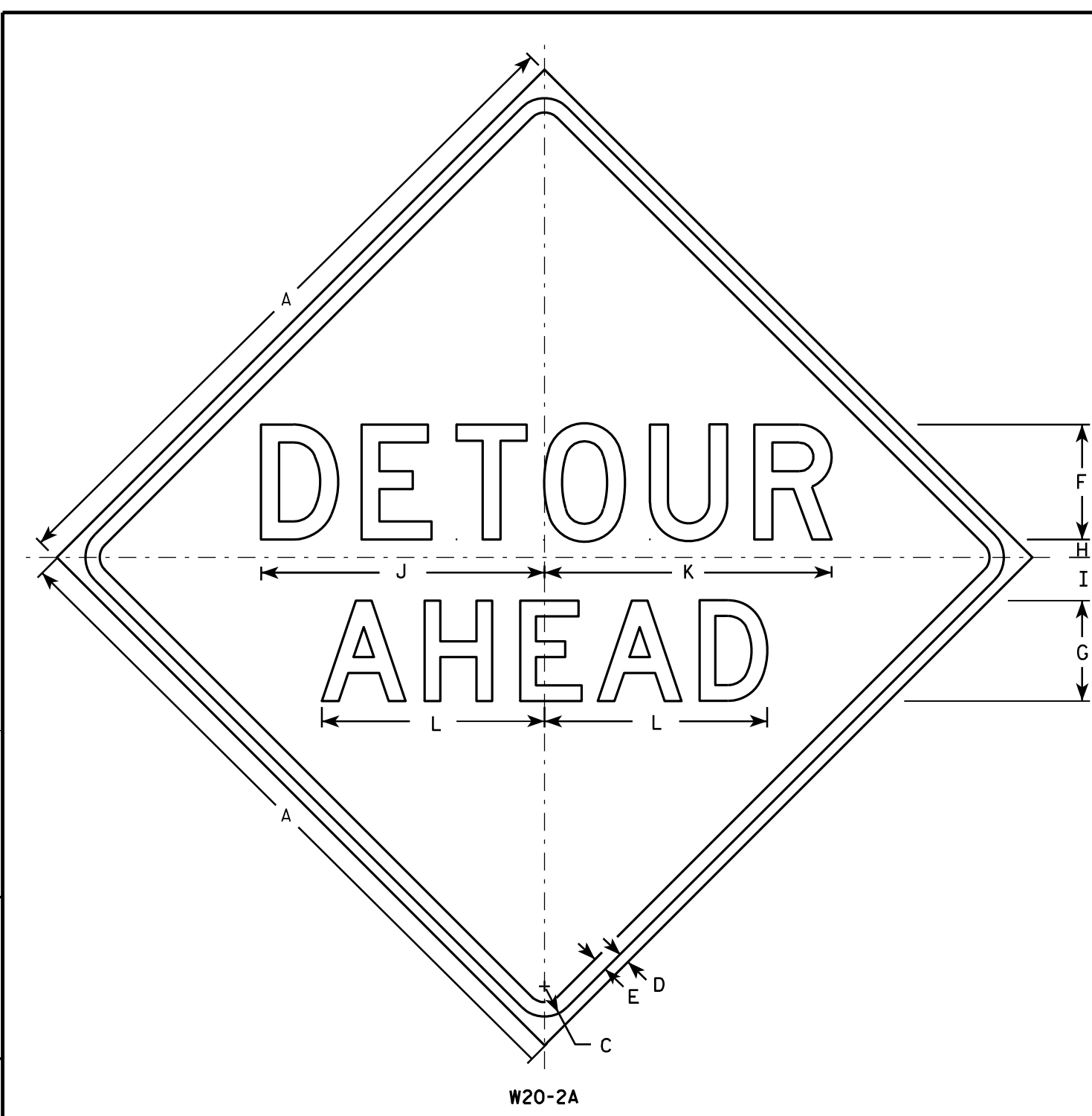
SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48		2 1/4	3/4	1	6	12	11 3/8	9 5/8	11 1/2	16	13	12	15 5/8	8	9 1/4	1 1/4	10 5/8								16.0	
2M	48		2 1/4	3/4	1	6	12	11 3/8	9 5/8	11 1/2	16	13	12	15 5/8	8	9 1/4	1 1/4	10 5/8								16.0	
3																											
4																											
5																											

STANDARD SIGN
W12-52

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 3/16/11 PLATE NO. W12-52.7



NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - See note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Line 1 is Series D.
Line 2 is Series D for AHEAD and Series C for all other distances.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	6	5	1	2 1/4	14 3/4	15	11 5/8	9	1 3/8	1 7/8	5 5/8	10 1/8	2 1/2	1 1/8	4 1/2	3 1/2	8	1 3/4	10 3/4			9.0
2S	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
2M	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
3	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
4	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0
5	48		2 1/4	3/4	1	8	7	1 1/4	3	19 3/4	20	15 1/2	12	1 7/8	2 5/8	7 1/2	13 1/2	3 3/8	1 1/2	6	4 5/8	10 5/8	2 3/8	14 3/8			16.0

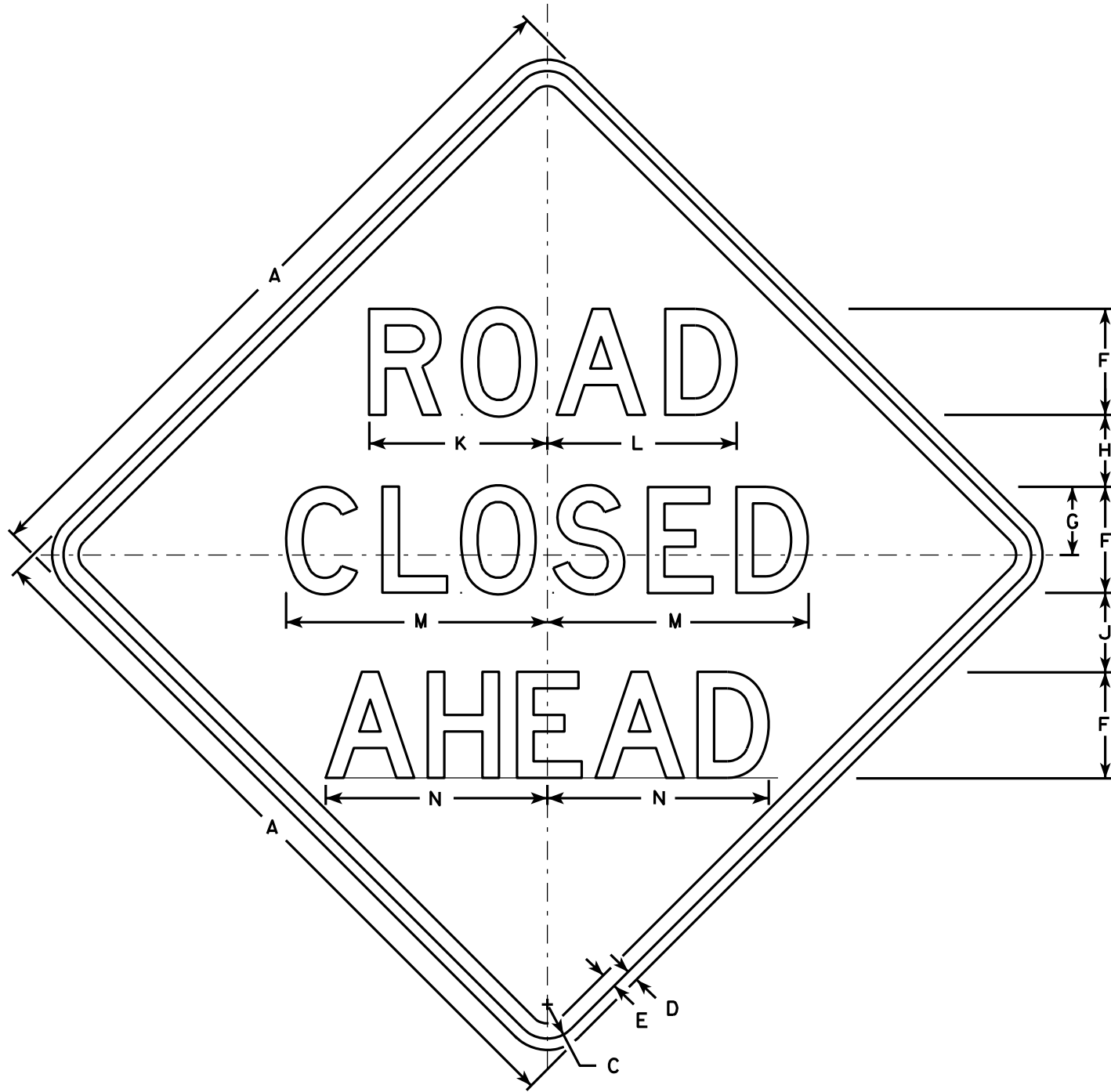
STANDARD SIGN
W20-2A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

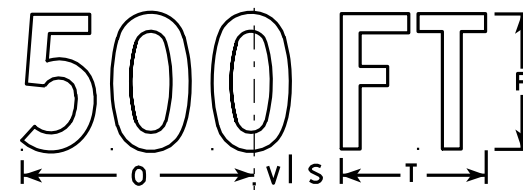
APPROVED *Matthew R. Raub*
for State Traffic Engineer

DATE 3/18/11 PLATE NO. W20-2.6

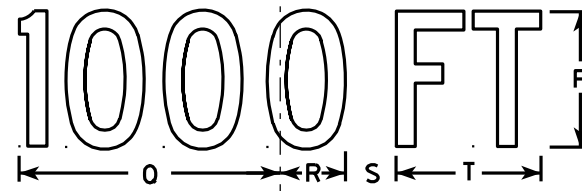
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E



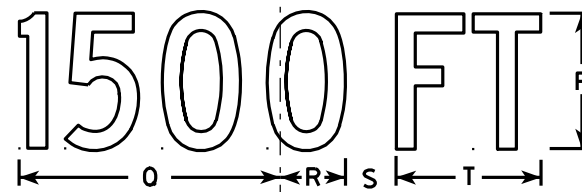
W20-3A



W20-3D



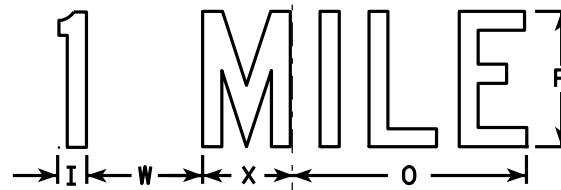
W20-3C



W20-3B



W20-3G



W20-3F

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Message Series - see note 5
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.
5. Lines 1 and 2 are Series D.
Line 3 is Series D for AHEAD and Series C for all other distances.

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1	36		1 5/8	5/8	3/4	5	3 3/8	3 1/2	1 1/8	4	8 3/8	8 7/8	12 1/2	11	9	6	10 1/8	2 1/2	1 7/8	5 5/8	8	1 3/8	4 1/2	3 1/2	10 3/4	1 3/4	9.0
2S	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
2M	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
3	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
4	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0
5	48		2 1/4	3/4	1	7	4 1/2	4 3/4	1 1/2	5 1/4	11 3/4	12 1/2	17 1/4	14 5/8	12	8	13 1/2	3 3/8	2 5/8	7 1/2	10 5/8	1 7/8	6	4 5/8	14 3/8	2 3/8	16.0

STANDARD SIGN
W20-3A, B, C, D, F & G

WISCONSIN DEPT OF TRANSPORTATION

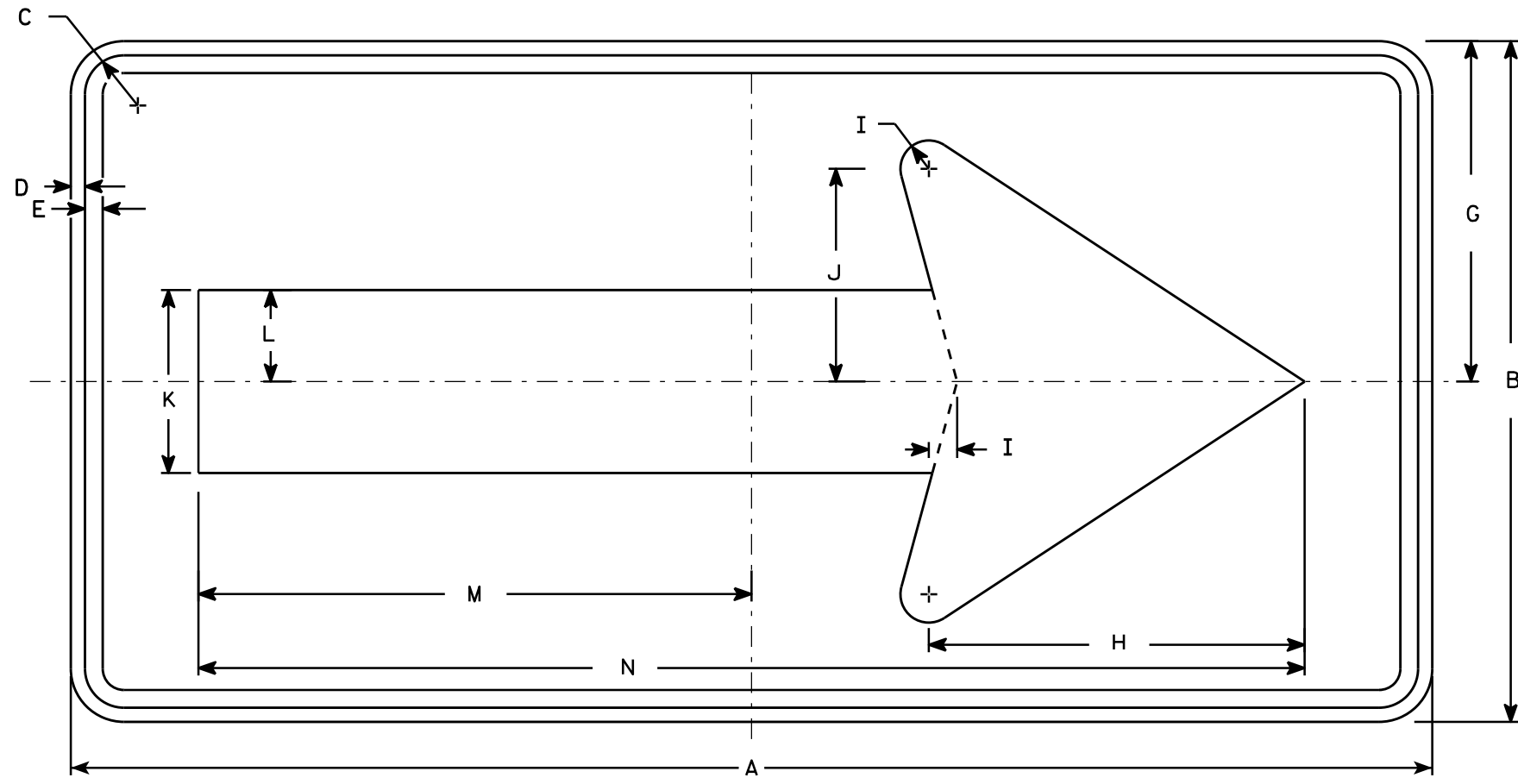
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 3/18/11 PLATE NO. W20-3.7

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ E

NOTES

1. Sign is Type II - Type F Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Orange
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



W01-6

7

7

SIZE	A	B	C	D	E	F	G	H	I	J	K	L	M	N	O	P	Q	R	S	T	U	V	W	X	Y	Z	Area sq. ft.
1																											
2S	48	24	1 3/8	1/2	5/8		12	13 1/4	1	7 1/2	6 1/2	3 1/4	19 1/2	39													8.0
2M	48	24	1 3/8	1/2	5/8		12	13 1/4	1	7 1/2	6 1/2	3 1/4	19 1/2	39													8.0
3	60	30	1 3/8	1/2	5/8		15	16 1/4	1 1/4	9 1/4	8	4	24 3/8	48 3/4													12.5
4	60	30	1 3/8	1/2	5/8		15	16 1/4	1 1/4	9 1/4	8	4	24 3/8	48 3/4													12.5
5	60	30	1 3/8	1/2	5/8		15	16 1/4	1 1/4	9 1/4	8	4	24 3/8	48 3/4													12.5

STANDARD SIGN
W01-6

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 11/18/13 PLATE NO. W01-6.1

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**



Wisconsin Department of Transportation

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<http://www.dot.wisconsin.gov>