

ORDER OF SHEETS

Section No.	1	Title
Section No.	2	Typical Sections and Details
Section No.	3	Estimate of Quantities
Section No.	3	Miscellaneous Quantities
Section No.	4	Right of Way Plat
Section No.	5	Plan and Profile
Section No.	6	Standard Detail Drawings
Section No.	7	Sign Plates
Section No.	8	Structure Plans
Section No.	9	Computer Earthwork Data
Section No.	9	Cross Sections

TOTAL SHEETS = 88



DESIGN DESIGNATION

A.A.D.T.	2024	=	3600
A.A.D.T.	2044	=	4050
D.H.V.		=	
D.D.		=	50/50
T.		=	8.5%
DESIGN SPEED		=	35 MPH
ESALS		=	1,100,000

CONVENTIONAL SYMBOLS

PLAN	
CORPORATE LIMITS	
PROPERTY LINE	
LOT LINE	
LIMITED HIGHWAY EASEMENT	
EXISTING RIGHT OF WAY	
PROPOSED OR NEW R/W LINE	
SLOPE INTERCEPT	
REFERENCE LINE	
EXISTING CULVERT	
PROPOSED CULVERT (Box or Pipe)	
COMBUSTIBLE FLUIDS	
MARSH AREA	
WOODED OR SHRUB AREA	

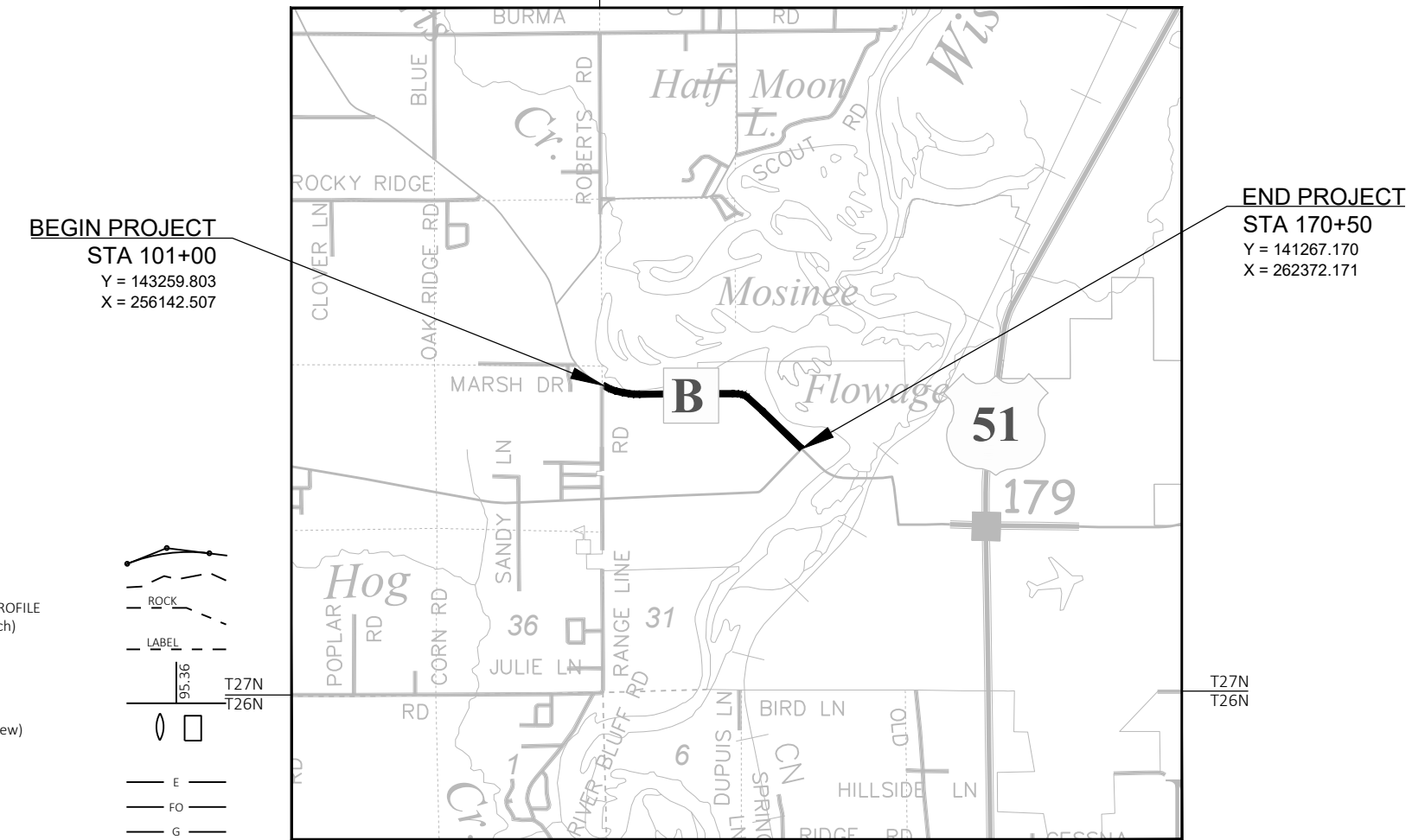


STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

C MOSINEE, MAIN STREET 4TH STREET TO RANGELINE ROAD LOCAL STREET MARATHON COUNTY

STATE PROJECT NUMBER
6999-16-73



BEGIN PROJECT
STA 101+00
Y = 143259.803
X = 256142.507

END PROJECT
STA 170+50
Y = 141267.170
X = 262372.171

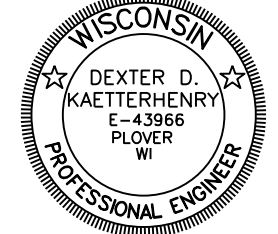
LAYOUT
SCALE 0 1 MI
TOTAL NET LENGTH OF CENTERLINE = 1.316 MI

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COORDINATE REFERENCE SYSTEM (WISCRS), MARATHON NAD83 (2011), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES. ELEVATIONS ARE REFERENCED TO NAVD 88 (2012). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 12A.

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
6999-16-73	WISC 2024257	1

APPROVED FOR
CITY OF MOSINEE
PROJECT ID: 6999-16-73
DATE: 11/1/2023
CITY OF MOSINEE MAYOR

ORIGINAL PLANS PREPARED BY
GREMME & ASSOCIATES, INC.
CONSULTING ENGINEERS
Stevens Point • Fond du Lac
120 Wiskho Boulevard North • Stevens Point, WI 54481
(715) 341-4363 • fax (715) 341-1256



DATE: 10/25/2023
DEXTER D. KAETTERHENRY, PE

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY
Surveyor: GREMME & ASSOCIATES, INC.
Designer: GREMME & ASSOCIATES, INC.
Project Manager: MICHAEL GRAGE
Regional Examiner: FRED SCHUNKE
Regional Supervisor: DANIEL ERVA

APPROVED FOR THE DEPARTMENT
DATE: 11/01/2023
(Signature)

E

GENERAL NOTES

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

A VERTICAL SAW CUT SHALL BE MADE THROUGH EXISTING DRIVEWAYS, SIDEWALKS, AND PAVEMENTS AT THE REMOVAL LIMITS.

SAW CUT LOCATIONS SHOWN ON THE PLANS ARE SUBJECT TO ADJUSTMENT BY THE ENGINEER IN THE FIELD.

WHEN THE QUANTITY OF THE ITEMS OF BASE AGGREGATE DENSE OR HMA PAVEMENT IS MEASURED FOR PAYMENT BY THE TON, THE ASPHALTIC SURFACE DEPTH OR THICKNESS OF THE MATERIAL SHOWN ON THE PLAN IS APPROXIMATE, AND THE ACTUAL THICKNESS WILL DEPEND ON THE DISTRIBUTION OF THE MATERIAL AS DIRECTED BY THE ENGINEER.

QUANTITY FOR PULVERIZED MATERIALS ARE MEASURED BY THE SY.

HMA PAVEMENT AND ASPHALTIC SURFACE WEIGHT CALCULATIONS ARE BASED ON 112 LBS/SY-INCH.

NO TREES OR SHRUBS ARE TO BE REMOVED WITHOUT THE APPROVAL OF THE ENGINEER.

ROADWAY DIMENSIONS ARE SHOWN TO THE FLANGE OF CURB.

ALL CURB RAMPS SHALL BE 6-INCH THICK AND PAID FOR AS CONCRETE SIDEWALK 6-INCH.

PAVE ASPHALT AS ONE LIFT. IF PULVERIZED MATERIAL IN THE FIELD IS MADE UP OF 25% OR LESS OF BASE MATERIAL TACK COAT IS REQ'D. ENGINEER IN FIELD TO DETERMINE RATIO.

ORDER OF SECTION 2 SHEETS

GENERAL NOTES

PROJECT OVERVIEW

TYPICAL SECTIONS

CONSTRUCTION DETAILS

CURB RAMP DETAILS

EROSION CONTROL PLAN

PAVEMENT MARKING

TRAFFIC CONTROL

DETOURS

PROJECT CONTACTS

ENGINEER

GREMME & ASSOCIATES, INC.

ATTN: DEXTER KAETTERHENRY, P.E.

120 WILSHIRE BOULEVARD NORTH

STEVENS POINT, WI 54481

OFFICE: 715.341.4363

EMAIL: d.kaetterhenry@gremmerassociates.com

CITY OF MOSINEE

ATTN: KEVIN BREIT, DIRECTOR OF PUBLIC WORKS

225 MAIN STREET

MOSINEE, WI 54455

OFFICE: 715.693.3840

EMAIL: publicworks@mosinee.wi.us

WISCONSIN DEPARTMENT OF NATURAL RESOURCES

ATTN: JAMES DOPERALSKI JR

2984 SHAWANO AVE

GREEN BAY, WI 54313

MOBILE: 920.412.0165

EMAIL: James.doperalski@wisconsin.gov

UTILITIES

COMMUNICATIONS

CHARTER COMMUNICATIONS

853 MCINTOSH STREET

WAUSAU, WI 54403

CONTACT: SCOTT OLSON

(715) 301-4075 (OFFICE)

(715) 302-1348 (MOBILE)

TDS TELECOM

171 PAOLI STREET

VERONA, WI 53594

CONTACT: JEFF OLSON

(608) 845-2219 (OFFICE)

(608) 444-6208 (MOBILE)

ELECTRIC

WISCONSIN PUBLIC SERVICE

1700 SHERMAN STREET

PO BOX 1166

WAUSAU, WI 54402

CONTACT: JARROD WURZ

(920) 848-7317 (OFFICE)

(715) 573-7806 (MOBILE)

EMAIL: jarrod.wurz@wisconsinpublicservice.com

GAS

WISCONSIN PUBLIC SERVICE

1700 SHERMAN STREET

WAUSAU, WI 54402

CONTACT: TYLER DAHLSTROM

(715) 848-7387 (OFFICE)

(715) 432-9212 (MOBILE)

EMAIL: tyler.dahlstrom@wisconsinpublicservice.com

SANITARY

CITY OF MOSINEE

DIRECTOR OF PUBLIC WORKS

225 MAIN STREET

MOSINEE, WI 54455

CONTACT: KEVIN BREIT

(715) 693-3840 (OFFICE)

(715) 693-1324 (FAX)

EMAIL: PUBLICWORKS@MOSINEE.WI.US

WATER

CITY OF MOSINEE

DIRECTOR OF PUBLIC WORKS

225 MAIN STREET

MOSINEE, WI 54455

CONTACT: KEVIN BREIT

(715) 693-3840 (OFFICE)

FAX (715) 693-1324

EMAIL: PUBLICWORKS@MOSINEE.WI.US

RUNOFF COEFFICIENT TABLE

	HYDROLOGIC SOIL GROUP											
	A			B			C			D		
	SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)			SLOPE RANGE (PERCENT)		
LAND USE:	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER	0-2	2-6	6 & OVER
ROW CROPS	.08 .22	.16 .30	.22 .38	.12 .26	.20 .34	.27 .44	.15 .30	.24 .37	.33 .50	.19 .34	.28 .41	.38 .56
MEDIAN STRIP-TURF	.19 .24	.20 .26	.24 .30	.19 .25	.22 .28	.26 .33	.20 .26	.23 .30	.30 .37	.20 .27	.25 .32	.30 .40
SIDE SLOPE-TURF			.25 .32			.27 .34			.28 .36			.30 .38
PAVEMENT:												
ASPHALT	.70 - .95											
CONCRETE	.80 - .95											
BRICK	.70 - .80											
DRIVES, WALKS	.75 - .85											
ROOFS	.75 - .95											
GRAVEL ROADS, SHOULDERS	.40 - .60											

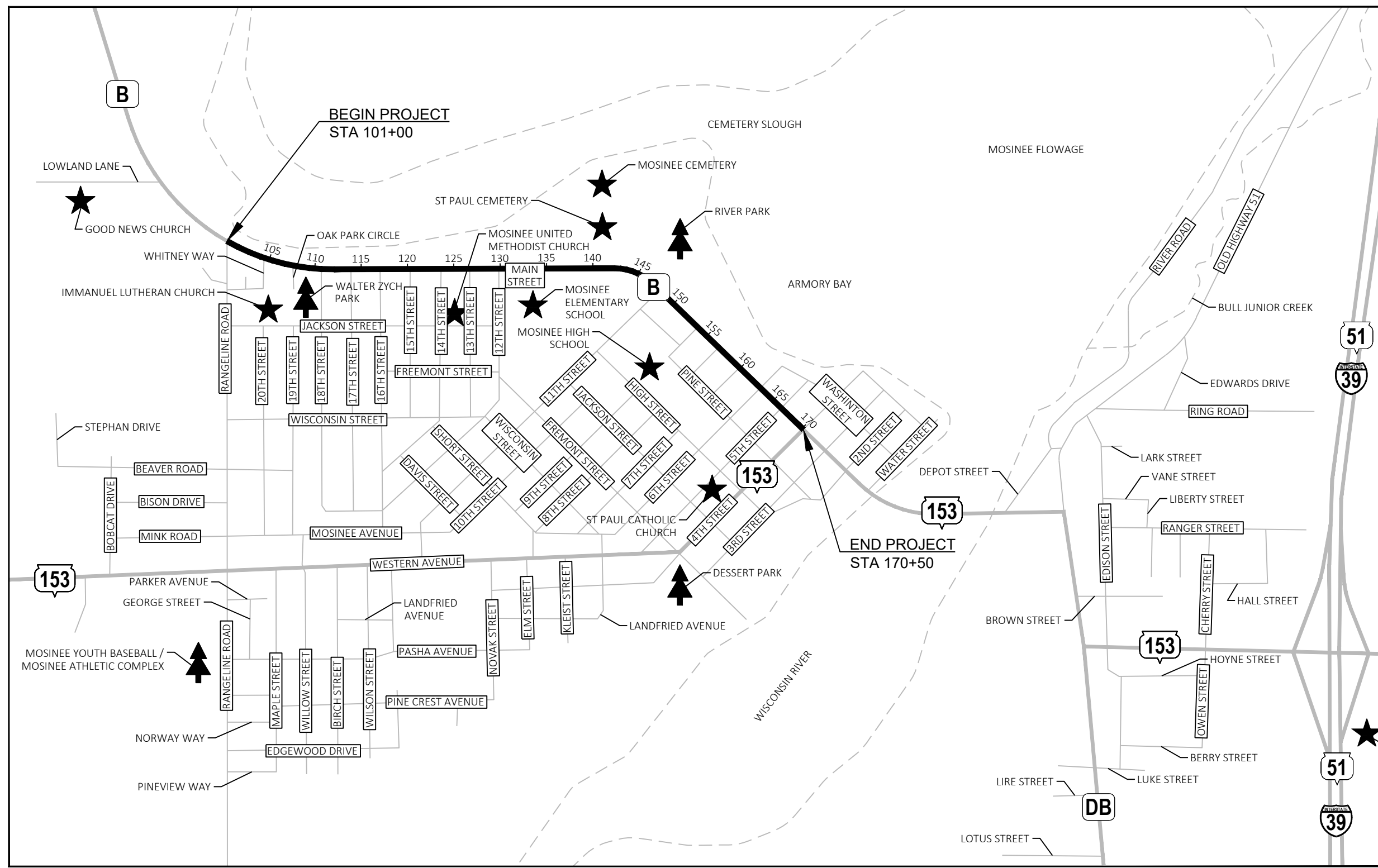
TOTAL PROJECT AREA = 5.95 ACRES
TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = 0.11 ACRES



Dial 811 or (800)242-8511

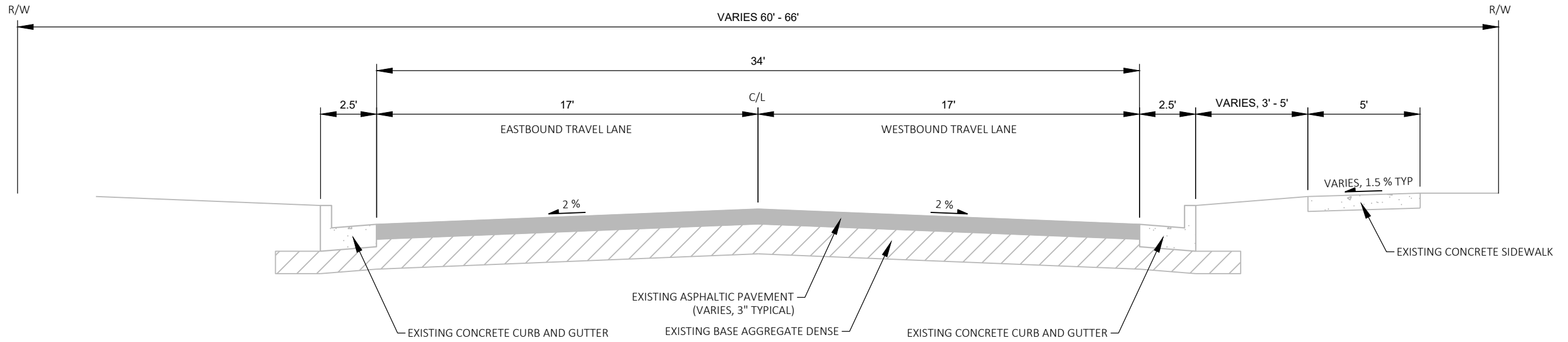
www.DiggersHotline.com

PROJECT LOCATION
MARATHON COUNTY
NC REGION - RHINELANDER OFFICE



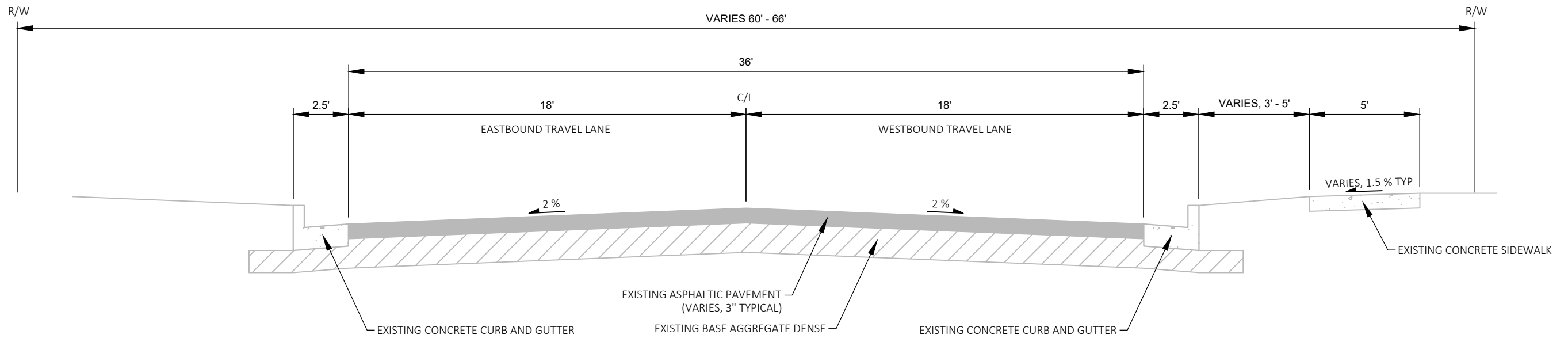
LEGEND

- PROJECT LIMITS
- PARK
- SIGNIFICANT LOCATION



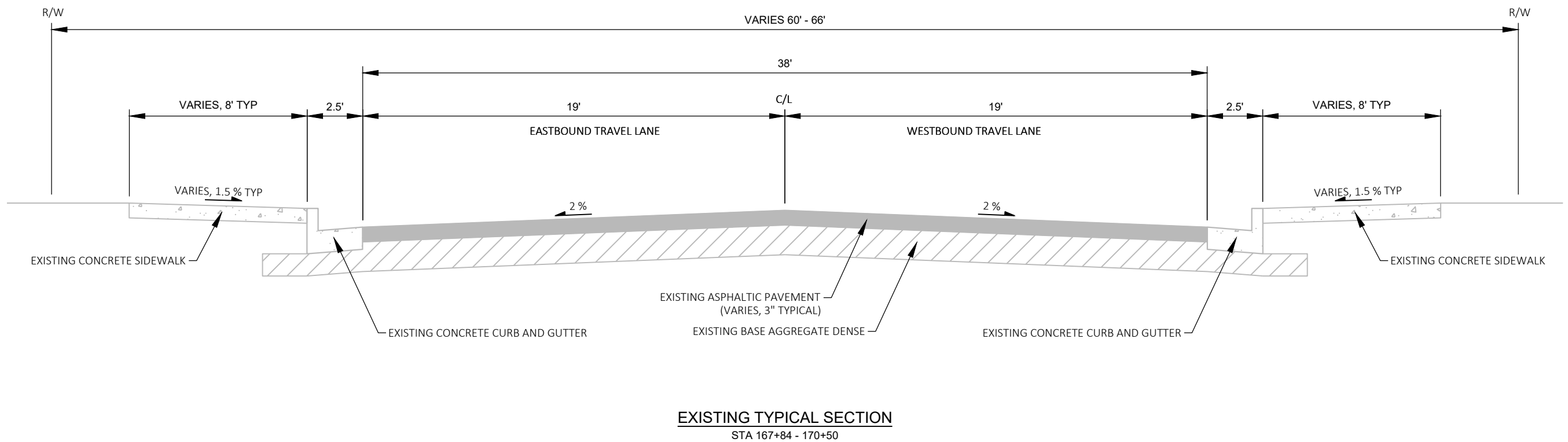
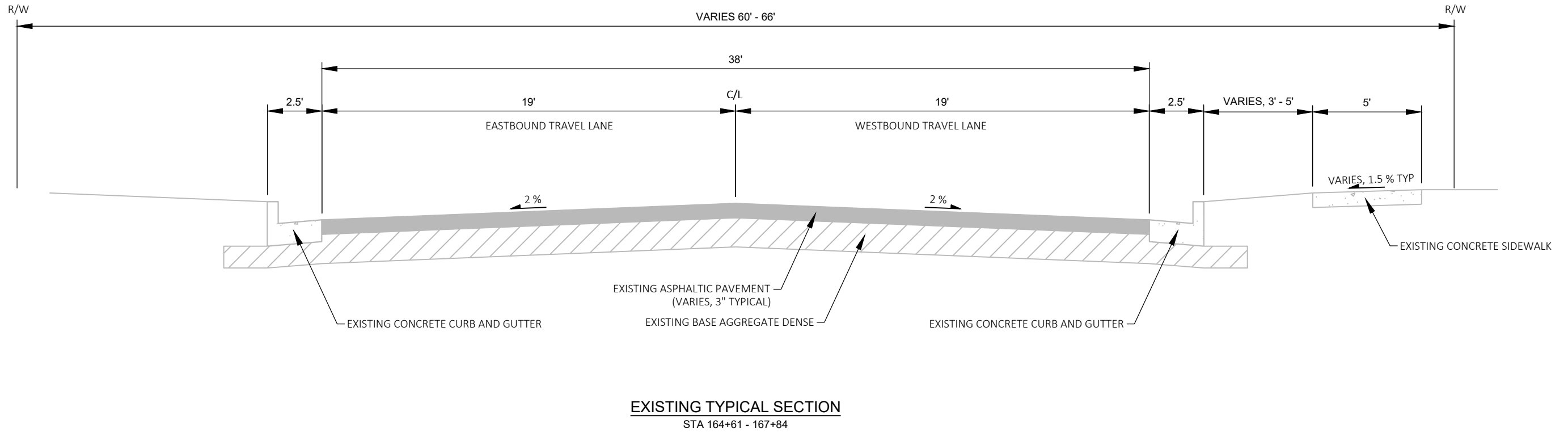
EXISTING TYPICAL SECTION

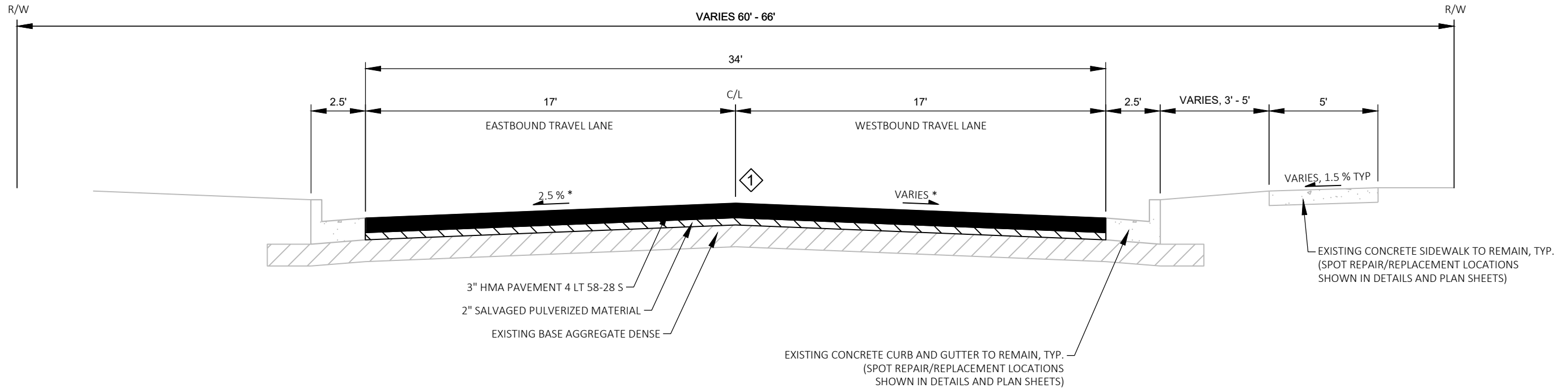
STA 101+00 - 161+24



EXISTING TYPICAL SECTION

STA 161+24 - 164+61

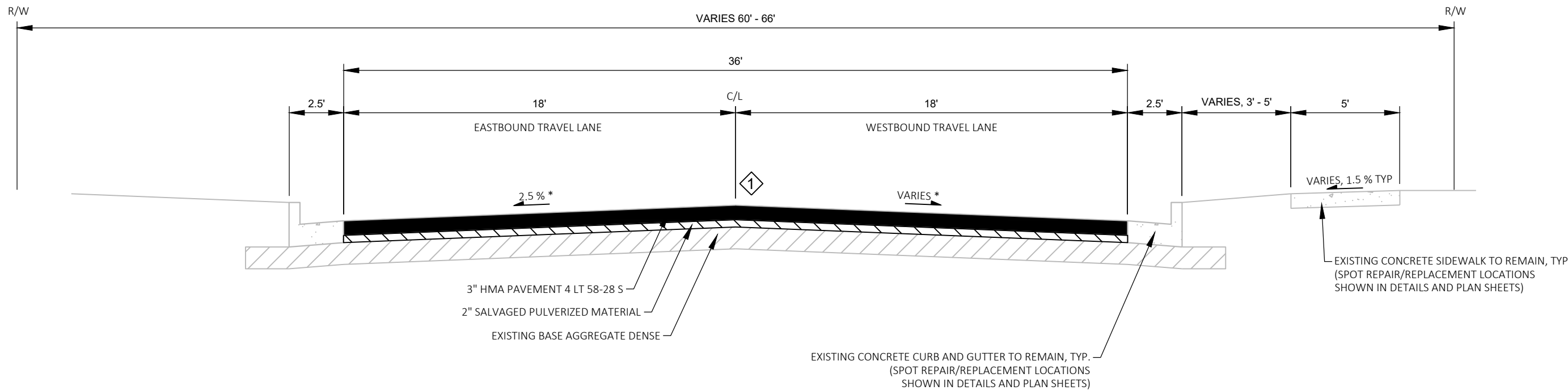




FINISHED TYPICAL SECTION

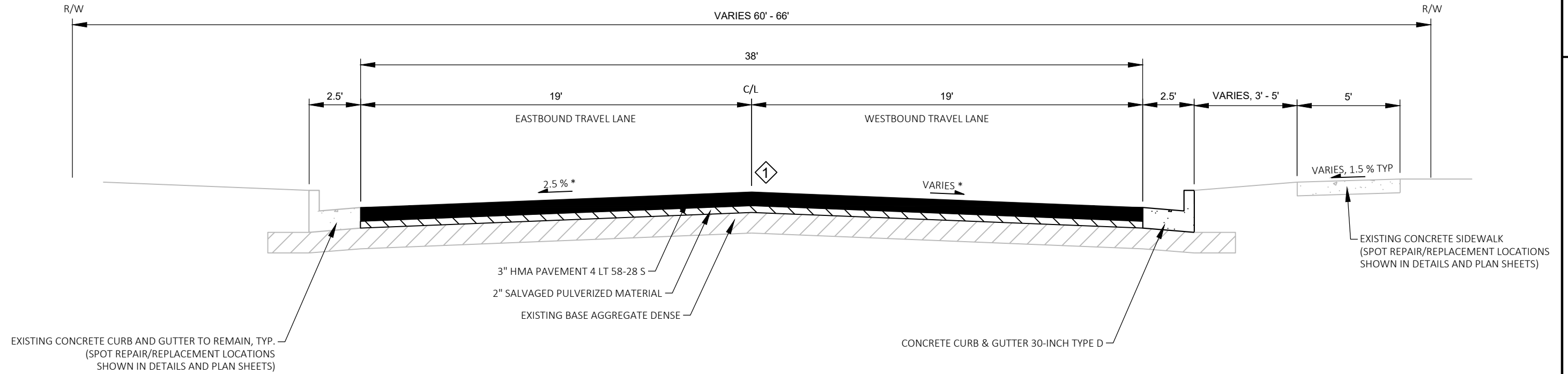
STA 101+00 - 161+41

- NOTES:**
- ◆ PULVERIZE AND RELAY
 - PULVERIZE TO A DEPTH OF 5" FROM FLANGE LINE TO FLANGE LINE (INCLUDES ALL EXISTING ASPHALT AND APPROXIMATELY 2" OF BASE MATERIAL)
 - * HAULING EXCESS MATERIAL
 - EXCESS PULVERIZED MATERIAL REQUIRED TO BE REMOVED FROM PROJECT.



FINISHED TYPICAL SECTION

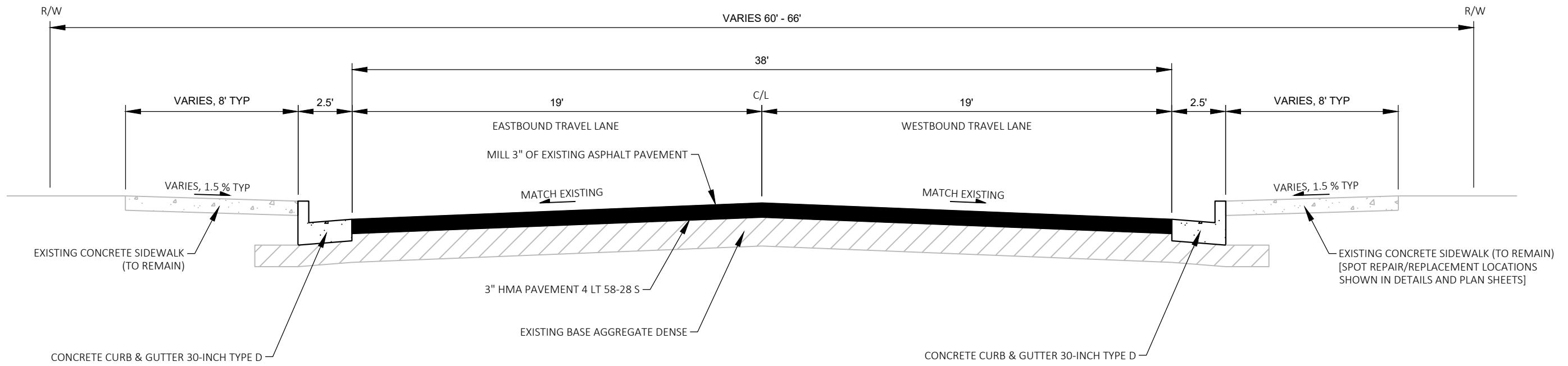
STA 161+41 - 164+61



FINISHED TYPICAL SECTION

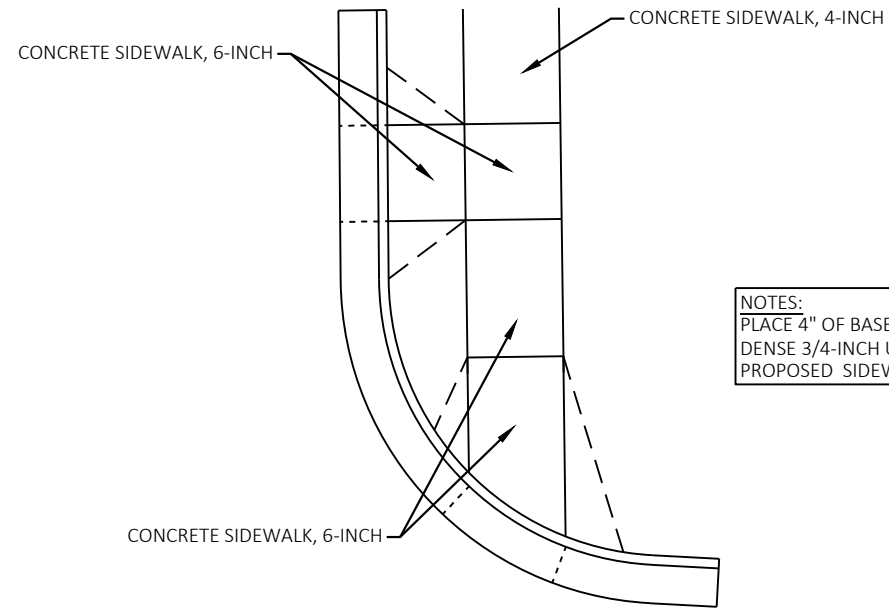
STA 164+61 - 168+00

- NOTES:**
- ① PULVERIZE AND RELAY
 - PULVERIZE TO A DEPTH OF 5" FROM FLANGE LINE TO FLANGE LINE (INCLUDES ALL EXISTING ASPHALT AND APPROXIMATELY 2" OF BASE MATERIAL)
 - * HAULING EXCESS MATERIAL
 - EXCESS PULVERIZED MATERIAL REQUIRED TO BE REMOVED FROM PROJECT.



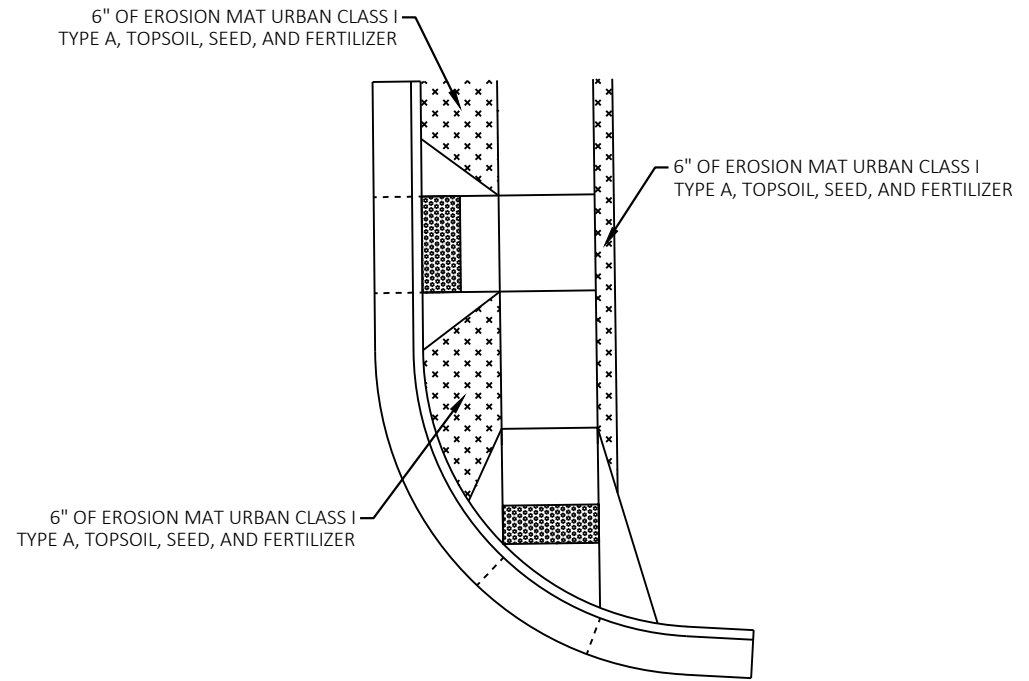
FINISHED TYPICAL SECTION

STA 168+00 - 170+50

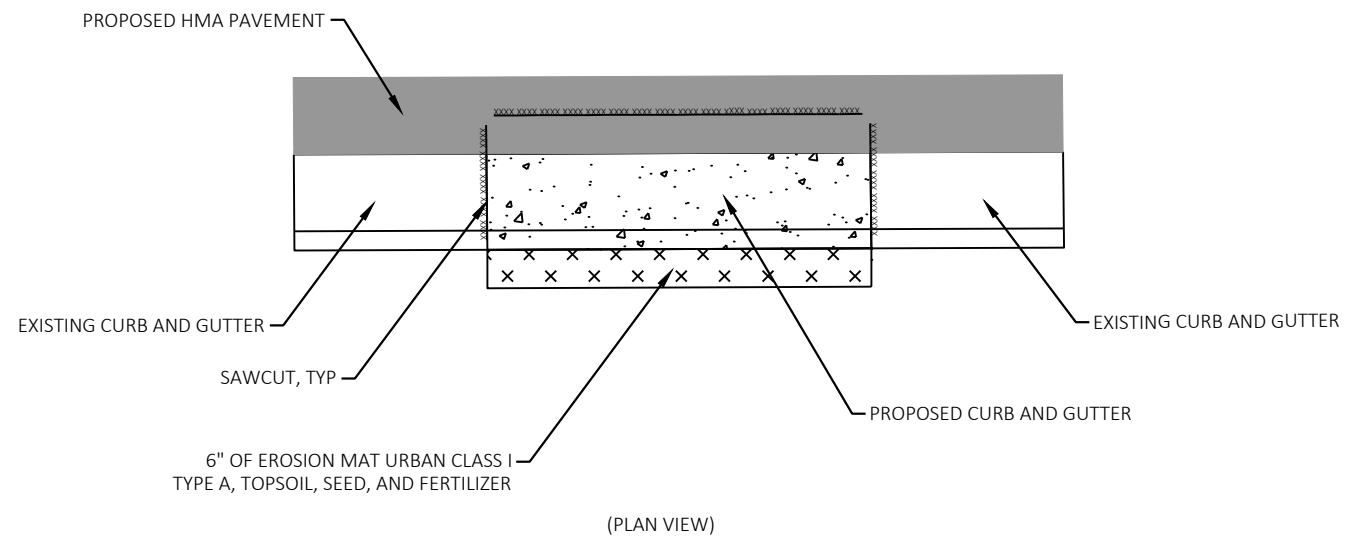


NOTES:
 PLACE 4" OF BASE AGGREGATE
 DENSE 3/4-INCH UNDER ALL
 PROPOSED SIDEWALK LOCATIONS.

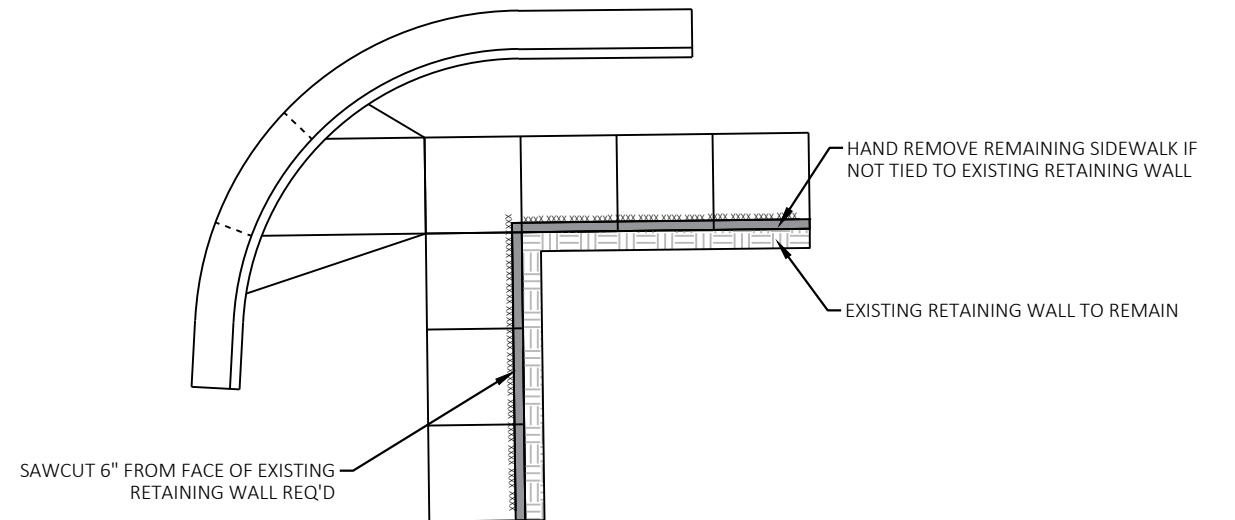
CONCRETE SIDEWALK AT CURB RAMPS DETAIL



EROSION MAT URBAN AT CURB RAMPS / CURB REPLACEMENT / SIDEWALK REPLACEMENT DETAIL



SPOT CURB AND GUTTER REPLACEMENT DETAIL



EXISTING RETAINING WALL AT CURB RAMPS REPLACEMENT DETAIL

LEGEND

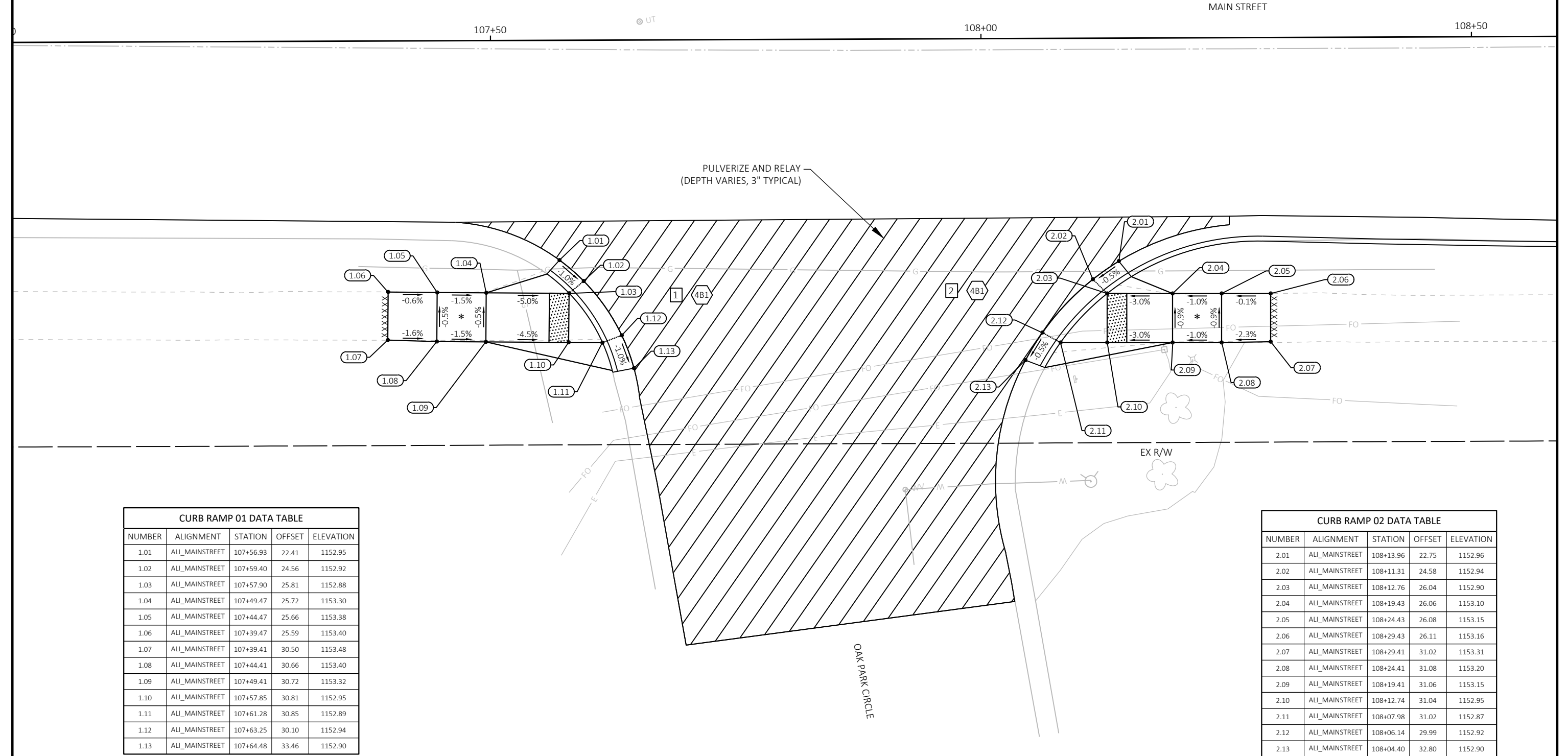
- 1 CONCRETE CURB RAMP TYPE 1
- 4A CONCRETE CURB RAMP TYPE 4A
- 4B1 CONCRETE CURB RAMP TYPE 4B1
- * LANDING PAD

NOTES:

REFER TO DETAILS "CURB RAMPS" FOR JOINT LOCATIONS AND ADDITIONAL DETAILS.

CURB RAMP TAPERS TO BE GRASS.

POINT ELEVATIONS AT FACE OF PEDESTRIAN CURB ARE SIDEWALK ELEVATIONS.



NUMBER	ALIGNMENT	STATION	OFFSET	ELEVATION
1.01	ALI_MAINSTREET	107+56.93	22.41	1152.95
1.02	ALI_MAINSTREET	107+59.40	24.56	1152.92
1.03	ALI_MAINSTREET	107+57.90	25.81	1152.88
1.04	ALI_MAINSTREET	107+49.47	25.72	1153.30
1.05	ALI_MAINSTREET	107+44.47	25.66	1153.38
1.06	ALI_MAINSTREET	107+39.47	25.59	1153.40
1.07	ALI_MAINSTREET	107+39.41	30.50	1153.48
1.08	ALI_MAINSTREET	107+44.41	30.66	1153.40
1.09	ALI_MAINSTREET	107+49.41	30.72	1153.32
1.10	ALI_MAINSTREET	107+57.85	30.81	1152.95
1.11	ALI_MAINSTREET	107+61.28	30.85	1152.89
1.12	ALI_MAINSTREET	107+63.25	30.10	1152.94
1.13	ALI_MAINSTREET	107+64.48	33.46	1152.90

NUMBER	ALIGNMENT	STATION	OFFSET	ELEVATION
2.01	ALI_MAINSTREET	108+13.96	22.75	1152.96
2.02	ALI_MAINSTREET	108+11.31	24.58	1152.94
2.03	ALI_MAINSTREET	108+12.76	26.04	1152.90
2.04	ALI_MAINSTREET	108+19.43	26.06	1153.10
2.05	ALI_MAINSTREET	108+24.43	26.08	1153.15
2.06	ALI_MAINSTREET	108+29.43	26.11	1153.16
2.07	ALI_MAINSTREET	108+29.41	31.02	1153.31
2.08	ALI_MAINSTREET	108+24.41	31.08	1153.20
2.09	ALI_MAINSTREET	108+19.41	31.06	1153.15
2.10	ALI_MAINSTREET	108+12.74	31.04	1152.95
2.11	ALI_MAINSTREET	108+07.98	31.02	1152.87
2.12	ALI_MAINSTREET	108+06.14	29.99	1152.92
2.13	ALI_MAINSTREET	108+04.40	32.80	1152.90

LEGEND

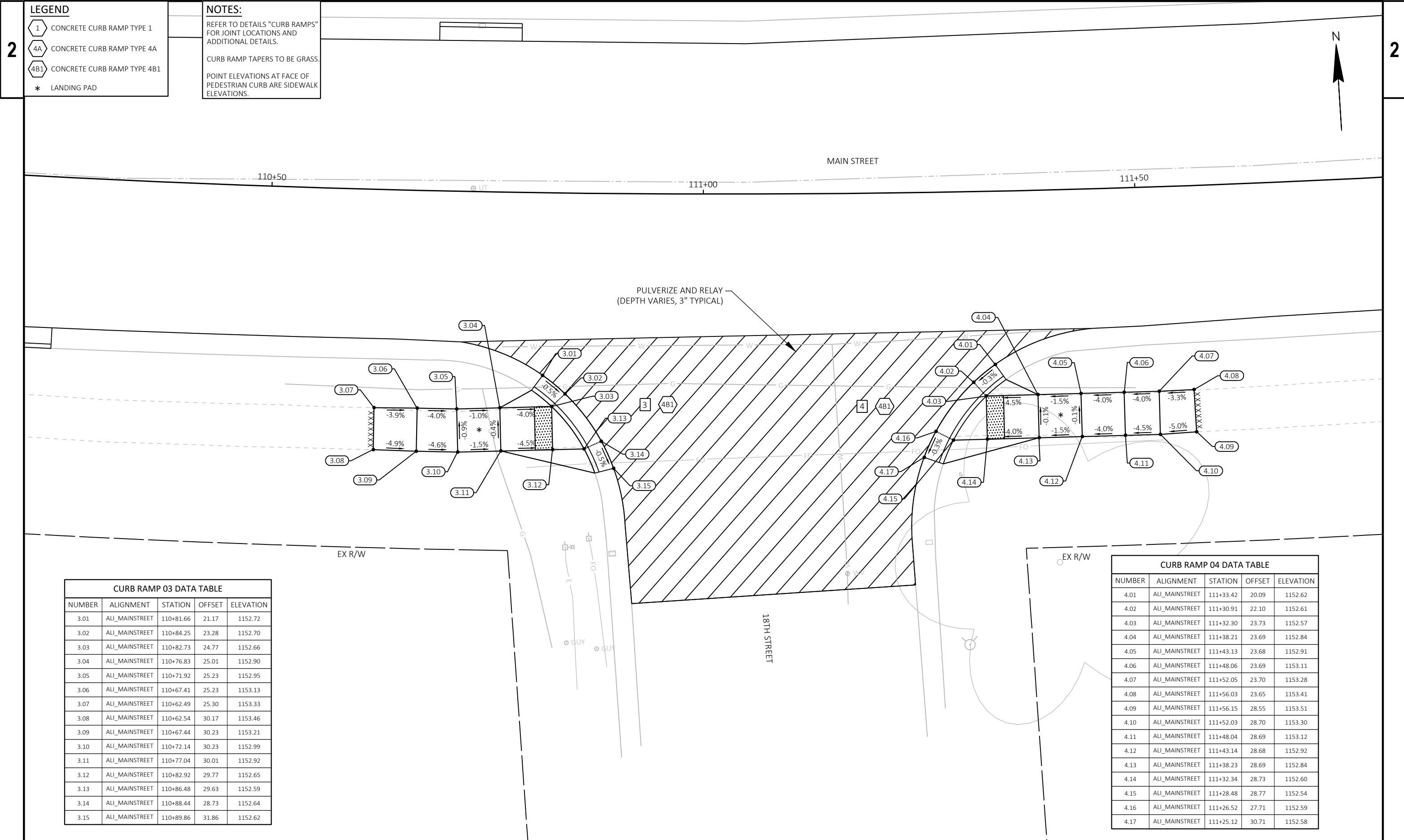
1	CONCRETE CURB RAMP TYPE 1
4A	CONCRETE CURB RAMP TYPE 4A
4B1	CONCRETE CURB RAMP TYPE 4B1
*	LANDING PAD

NOTES:

REFER TO DETAILS "CURB RAMPS" FOR JOINT LOCATIONS AND ADDITIONAL DETAILS.

CURB RAMP TAPERS TO BE GRASS.

POINT ELEVATIONS AT FACE OF PEDESTRIAN CURB ARE SIDEWALK ELEVATIONS.



CURB RAMP 03 DATA TABLE

NUMBER	ALIGNMENT	STATION	OFFSET	ELEVATION
3.01	ALI_MAINSTREET	110+81.66	21.17	1152.72
3.02	ALI_MAINSTREET	110+84.25	23.28	1152.70
3.03	ALI_MAINSTREET	110+82.73	24.77	1152.66
3.04	ALI_MAINSTREET	110+76.83	25.01	1152.90
3.05	ALI_MAINSTREET	110+71.92	25.23	1152.95
3.06	ALI_MAINSTREET	110+67.41	25.23	1153.13
3.07	ALI_MAINSTREET	110+62.49	25.30	1153.33
3.08	ALI_MAINSTREET	110+62.54	30.17	1153.46
3.09	ALI_MAINSTREET	110+67.44	30.23	1153.21
3.10	ALI_MAINSTREET	110+72.14	30.23	1152.99
3.11	ALI_MAINSTREET	110+77.04	30.01	1152.92
3.12	ALI_MAINSTREET	110+82.92	29.77	1152.65
3.13	ALI_MAINSTREET	110+86.48	29.63	1152.59
3.14	ALI_MAINSTREET	110+88.44	28.73	1152.64
3.15	ALI_MAINSTREET	110+89.86	31.86	1152.62

CURB RAMP 04 DATA TABLE

NUMBER	ALIGNMENT	STATION	OFFSET	ELEVATION
4.01	ALI_MAINSTREET	111+33.42	20.09	1152.62
4.02	ALI_MAINSTREET	111+30.91	22.10	1152.61
4.03	ALI_MAINSTREET	111+32.30	23.73	1152.57
4.04	ALI_MAINSTREET	111+38.21	23.69	1152.84
4.05	ALI_MAINSTREET	111+43.13	23.68	1152.91
4.06	ALI_MAINSTREET	111+48.06	23.69	1153.11
4.07	ALI_MAINSTREET	111+52.05	23.70	1153.28
4.08	ALI_MAINSTREET	111+56.03	23.65	1153.41
4.09	ALI_MAINSTREET	111+56.15	28.55	1153.51
4.10	ALI_MAINSTREET	111+52.03	28.70	1153.30
4.11	ALI_MAINSTREET	111+48.04	28.69	1153.12
4.12	ALI_MAINSTREET	111+43.14	28.68	1152.92
4.13	ALI_MAINSTREET	111+38.23	28.69	1152.84
4.14	ALI_MAINSTREET	111+32.34	28.73	1152.60
4.15	ALI_MAINSTREET	111+28.48	28.77	1152.54
4.16	ALI_MAINSTREET	111+26.52	27.71	1152.59
4.17	ALI_MAINSTREET	111+25.12	30.71	1152.58

LEGEND

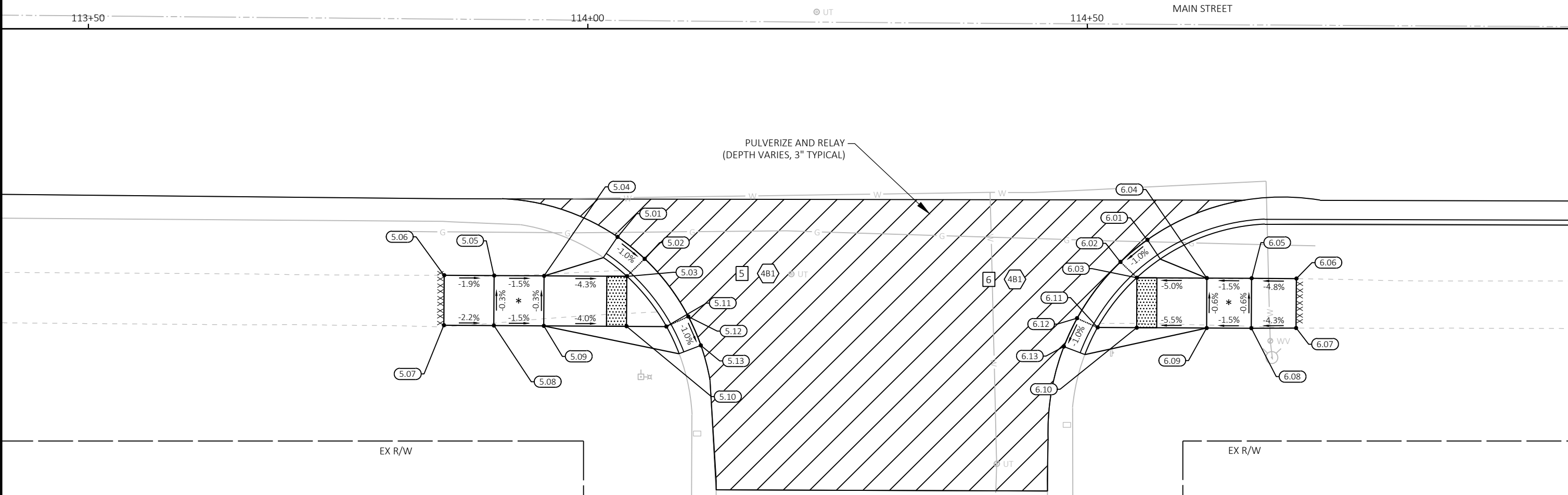
- 1 CONCRETE CURB RAMP TYPE 1
- 4A CONCRETE CURB RAMP TYPE 4A
- 4B1 CONCRETE CURB RAMP TYPE 4B1
- * LANDING PAD

NOTES:

REFER TO DETAILS "CURB RAMPS" FOR JOINT LOCATIONS AND ADDITIONAL DETAILS.

CURB RAMP TAPERS TO BE GRASS.

POINT ELEVATIONS AT FACE OF PEDESTRIAN CURB ARE SIDEWALK ELEVATIONS.



CURB RAMP 05 DATA TABLE

NUMBER	ALIGNMENT	STATION	OFFSET	ELEVATION
5.01	ALI_MAINSTREET	114+02.96	20.84	1153.47
5.02	ALI_MAINSTREET	114+05.68	23.03	1153.44
5.03	ALI_MAINSTREET	114+03.89	24.77	1153.39
5.04	ALI_MAINSTREET	113+95.61	24.73	1153.74
5.05	ALI_MAINSTREET	113+90.61	24.70	1153.81
5.06	ALI_MAINSTREET	113+85.61	24.67	1153.90
5.07	ALI_MAINSTREET	113+85.58	29.67	1153.94
5.08	ALI_MAINSTREET	113+90.58	29.70	1153.83
5.09	ALI_MAINSTREET	113+95.58	29.73	1153.75
5.10	ALI_MAINSTREET	114+03.86	29.77	1153.42
5.11	ALI_MAINSTREET	114+07.85	29.80	1153.36
5.12	ALI_MAINSTREET	114+10.04	28.80	1153.41
5.13	ALI_MAINSTREET	114+11.29	31.69	1153.38

CURB RAMP 06 DATA TABLE

NUMBER	ALIGNMENT	STATION	OFFSET	ELEVATION
6.01	ALI_MAINSTREET	114+56.00	21.13	1153.41
6.02	ALI_MAINSTREET	114+53.31	23.34	1153.38
6.03	ALI_MAINSTREET	114+54.96	24.91	1153.33
6.04	ALI_MAINSTREET	114+61.96	24.95	1153.68
6.05	ALI_MAINSTREET	114+66.44	24.98	1153.75
6.06	ALI_MAINSTREET	114+70.92	25.01	1153.96
6.07	ALI_MAINSTREET	114+70.89	29.95	1153.97
6.08	ALI_MAINSTREET	114+66.41	29.98	1153.78
6.09	ALI_MAINSTREET	114+61.93	29.95	1153.71
6.10	ALI_MAINSTREET	114+54.93	29.91	1153.32
6.11	ALI_MAINSTREET	114+51.03	29.89	1153.27
6.12	ALI_MAINSTREET	114+48.96	28.97	1153.31
6.13	ALI_MAINSTREET	114+47.64	31.80	1153.28

LEGEND

- 1 CONCRETE CURB RAMP TYPE 1
- 4A CONCRETE CURB RAMP TYPE 4A
- 4B1 CONCRETE CURB RAMP TYPE 4B1
- * LANDING PAD

NOTES:

REFER TO DETAILS "CURB RAMP" FOR JOINT LOCATIONS AND ADDITIONAL DETAILS.

CURB RAMP TAPERS TO BE GRASS.

POINT ELEVATIONS AT FACE OF PEDESTRIAN CURB ARE SIDEWALK ELEVATIONS.

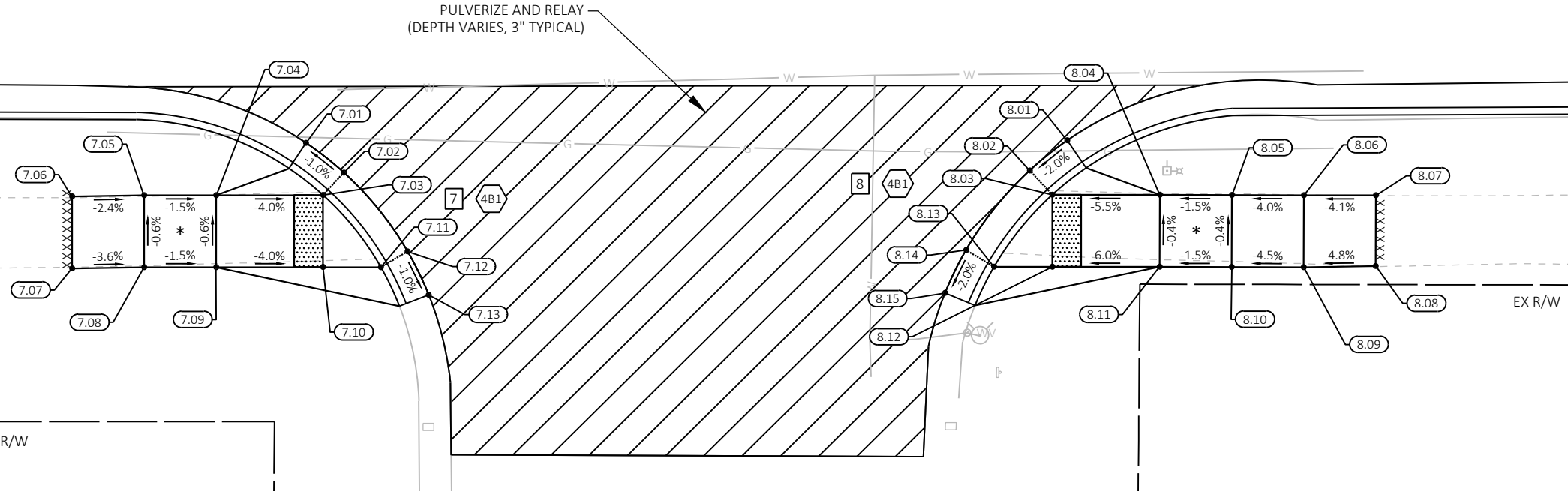


117+00

117+50

MAIN STREET

118+00



CURB RAMP 07 DATA TABLE				
NUMBER	ALIGNMENT	STATION	OFFSET	ELEVATION
7.01	ALI_MAINSTREET	117+27.79	21.90	1154.14
7.02	ALI_MAINSTREET	117+30.42	23.96	1154.17
7.03	ALI_MAINSTREET	117+28.97	25.52	1154.13
7.04	ALI_MAINSTREET	117+21.56	25.54	1154.43
7.05	ALI_MAINSTREET	117+16.56	25.54	1154.50
7.06	ALI_MAINSTREET	117+11.56	25.62	1154.62
7.07	ALI_MAINSTREET	117+11.56	30.62	1154.71
7.08	ALI_MAINSTREET	117+16.56	30.54	1154.53
7.09	ALI_MAINSTREET	117+21.56	30.54	1154.46
7.10	ALI_MAINSTREET	117+28.97	30.52	1154.16
7.11	ALI_MAINSTREET	117+32.98	30.53	1154.14
7.12	ALI_MAINSTREET	117+34.84	29.43	1154.18
7.13	ALI_MAINSTREET	117+36.31	32.45	1154.15

CURB RAMP 08 DATA TABLE				
NUMBER	ALIGNMENT	STATION	OFFSET	ELEVATION
8.01	ALI_MAINSTREET	117+80.65	21.72	1154.33
8.02	ALI_MAINSTREET	117+78.04	23.81	1154.26
8.03	ALI_MAINSTREET	117+79.60	25.50	1154.22
8.04	ALI_MAINSTREET	117+87.07	25.50	1154.63
8.05	ALI_MAINSTREET	117+92.07	25.52	1154.70
8.06	ALI_MAINSTREET	117+97.07	25.53	1154.90
8.07	ALI_MAINSTREET	118+02.07	25.54	1155.11
8.08	ALI_MAINSTREET	118+02.06	30.45	1155.19
8.09	ALI_MAINSTREET	117+97.06	30.53	1154.95
8.10	ALI_MAINSTREET	117+92.06	30.52	1154.72
8.11	ALI_MAINSTREET	117+87.06	30.50	1154.65
8.12	ALI_MAINSTREET	117+79.61	30.50	1154.20
8.13	ALI_MAINSTREET	117+75.56	30.49	1154.14
8.14	ALI_MAINSTREET	117+73.62	29.34	1154.19
8.15	ALI_MAINSTREET	117+72.16	32.32	1154.12

PROJECT NO: 6999-16-73

ROADWAY: MAIN STREET

COUNTY: MARATHON

CURB RAMP DETAILS

SHEET

E

LEGEND

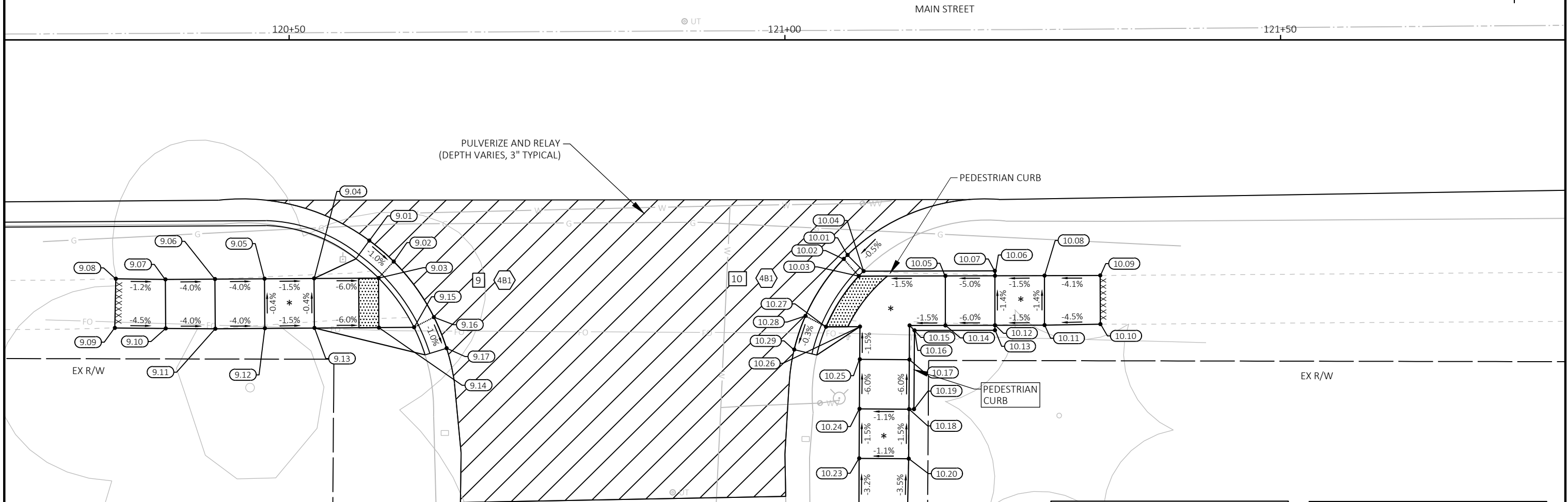
1	CONCRETE CURB RAMP TYPE 1
4A	CONCRETE CURB RAMP TYPE 4A
4B1	CONCRETE CURB RAMP TYPE 4B1
*	LANDING PAD

NOTES:

REFER TO DETAILS "CURB RAMP" FOR JOINT LOCATIONS AND ADDITIONAL DETAILS.

CURB RAMP TAPERS TO BE GRASS.

POINT ELEVATIONS AT FACE OF PEDESTRIAN CURB ARE SIDEWALK ELEVATIONS.



CURB RAMP 09 DATA TABLE

NUMBER	ALIGNMENT	STATION	OFFSET	ELEVATION
9.01	ALI_MAINSTREET	120+58.07	20.26	1153.83
9.02	ALI_MAINSTREET	120+60.53	22.38	1153.80
9.03	ALI_MAINSTREET	120+59.02	24.05	1153.75
9.04	ALI_MAINSTREET	120+52.52	24.09	1154.14
9.05	ALI_MAINSTREET	120+47.52	24.12	1154.22
9.06	ALI_MAINSTREET	120+42.52	24.16	1154.42
9.07	ALI_MAINSTREET	120+37.52	24.19	1154.62
9.08	ALI_MAINSTREET	120+32.51	24.11	1154.68
9.09	ALI_MAINSTREET	120+32.41	29.08	1154.87
9.10	ALI_MAINSTREET	120+37.55	29.14	1154.64
9.11	ALI_MAINSTREET	120+42.55	29.16	1154.44
9.12	ALI_MAINSTREET	120+47.55	29.12	1154.24
9.13	ALI_MAINSTREET	120+52.55	29.09	1154.16
9.14	ALI_MAINSTREET	120+59.05	29.05	1153.77
9.15	ALI_MAINSTREET	120+62.59	29.02	1153.72
9.16	ALI_MAINSTREET	120+64.60	28.00	1153.76
9.17	ALI_MAINSTREET	120+65.87	31.13	1153.73

RADIAL WARNING FIELD

LANDING "XR"	12.0'
AREA	12.6 SF
LONG CHORD	6.1'
BOC RADIUS	17.6'

CURB RAMP 10 DATA TABLE

NUMBER	ALIGNMENT	STATION	OFFSET	ELEVATION
10.01	ALI_MAINSTREET	121+06.33	21.72	1153.80
10.02	ALI_MAINSTREET	121+05.94	22.12	1153.80
10.03	ALI_MAINSTREET	121+07.46	23.85	1153.75
10.04	ALI_MAINSTREET	121+07.94	23.35	1154.25
10.05	ALI_MAINSTREET	121+16.18	23.83	1153.88
10.06	ALI_MAINSTREET	121+21.18	23.81	1154.13
10.07	ALI_MAINSTREET	121+21.18	23.31	1154.13
10.08	ALI_MAINSTREET	121+26.18	23.80	1154.21
10.09	ALI_MAINSTREET	121+31.81	23.79	1154.44
10.10	ALI_MAINSTREET	121+31.82	28.70	1154.53
10.11	ALI_MAINSTREET	121+26.20	28.80	1154.27
10.12	ALI_MAINSTREET	121+21.20	28.81	1154.20
10.13	ALI_MAINSTREET	121+21.20	29.31	1154.20
10.14	ALI_MAINSTREET	121+16.20	28.83	1153.90
10.15	ALI_MAINSTREET	121+12.57	28.84	1153.85

CURB RAMP 10 DATA TABLE

NUMBER	ALIGNMENT	STATION	OFFSET	ELEVATION
10.16	ALI_MAINSTREET	121+13.07	29.33	1154.35
10.17	ALI_MAINSTREET	121+12.55	32.28	1153.90
10.18	ALI_MAINSTREET	121+12.53	37.28	1154.20
10.19	ALI_MAINSTREET	121+13.02	37.28	1154.20
10.20	ALI_MAINSTREET	121+12.50	42.28	1154.27
10.21	ALI_MAINSTREET	121+12.38	47.60	1154.46
10.22	ALI_MAINSTREET	121+07.47	47.55	1154.39
10.23	ALI_MAINSTREET	121+07.50	42.25	1154.22
10.24	ALI_MAINSTREET	121+07.53	37.25	1154.14
10.25	ALI_MAINSTREET	121+07.55	32.25	1153.84
10.26	ALI_MAINSTREET	121+07.57	28.95	1153.79
10.27	ALI_MAINSTREET	121+04.15	28.96	1153.74
10.28	ALI_MAINSTREET	121+02.08	27.87	1153.79
10.29	ALI_MAINSTREET	121+00.94	31.27	1153.78

LEGEND

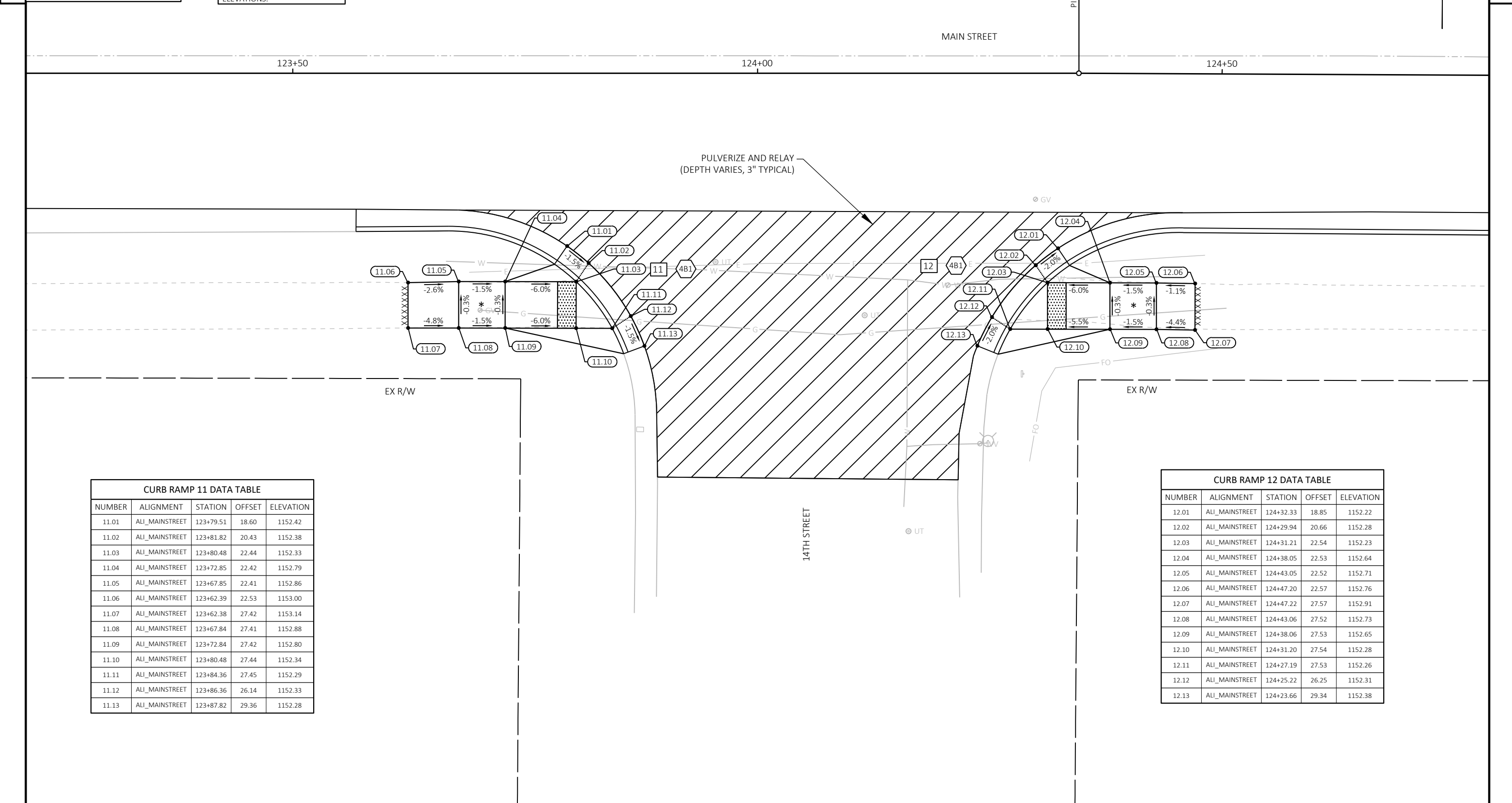
- 1 CONCRETE CURB RAMP TYPE 1
- 4A CONCRETE CURB RAMP TYPE 4A
- 4B1 CONCRETE CURB RAMP TYPE 4B1
- * LANDING PAD

NOTES:

REFER TO DETAILS "CURB RAMPS" FOR JOINT LOCATIONS AND ADDITIONAL DETAILS.

CURB RAMP TAPERS TO BE GRASS.

POINT ELEVATIONS AT FACE OF PEDESTRIAN CURB ARE SIDEWALK ELEVATIONS.



CURB RAMP 11 DATA TABLE

NUMBER	ALIGNMENT	STATION	OFFSET	ELEVATION
11.01	ALI_MAINSTREET	123+79.51	18.60	1152.42
11.02	ALI_MAINSTREET	123+81.82	20.43	1152.38
11.03	ALI_MAINSTREET	123+80.48	22.44	1152.33
11.04	ALI_MAINSTREET	123+72.85	22.42	1152.79
11.05	ALI_MAINSTREET	123+67.85	22.41	1152.86
11.06	ALI_MAINSTREET	123+62.39	22.53	1153.00
11.07	ALI_MAINSTREET	123+62.38	27.42	1153.14
11.08	ALI_MAINSTREET	123+67.84	27.41	1152.88
11.09	ALI_MAINSTREET	123+72.84	27.42	1152.80
11.10	ALI_MAINSTREET	123+80.48	27.44	1152.34
11.11	ALI_MAINSTREET	123+84.36	27.45	1152.29
11.12	ALI_MAINSTREET	123+86.36	26.14	1152.33
11.13	ALI_MAINSTREET	123+87.82	29.36	1152.28

CURB RAMP 12 DATA TABLE

NUMBER	ALIGNMENT	STATION	OFFSET	ELEVATION
12.01	ALI_MAINSTREET	124+32.33	18.85	1152.22
12.02	ALI_MAINSTREET	124+29.94	20.66	1152.28
12.03	ALI_MAINSTREET	124+31.21	22.54	1152.23
12.04	ALI_MAINSTREET	124+38.05	22.53	1152.64
12.05	ALI_MAINSTREET	124+43.05	22.52	1152.71
12.06	ALI_MAINSTREET	124+47.20	22.57	1152.76
12.07	ALI_MAINSTREET	124+47.22	27.57	1152.91
12.08	ALI_MAINSTREET	124+43.06	27.52	1152.73
12.09	ALI_MAINSTREET	124+38.06	27.53	1152.65
12.10	ALI_MAINSTREET	124+31.20	27.54	1152.28
12.11	ALI_MAINSTREET	124+27.19	27.53	1152.26
12.12	ALI_MAINSTREET	124+25.22	26.25	1152.31
12.13	ALI_MAINSTREET	124+23.66	29.34	1152.38

LEGEND

1	CONCRETE CURB RAMP TYPE 1
4A	CONCRETE CURB RAMP TYPE 4A
4B1	CONCRETE CURB RAMP TYPE 4B1
*	LANDING PAD

NOTES:

REFER TO DETAILS "CURB RAMPS" FOR JOINT LOCATIONS AND ADDITIONAL DETAILS.

CURB RAMP TAPERS TO BE GRASS.

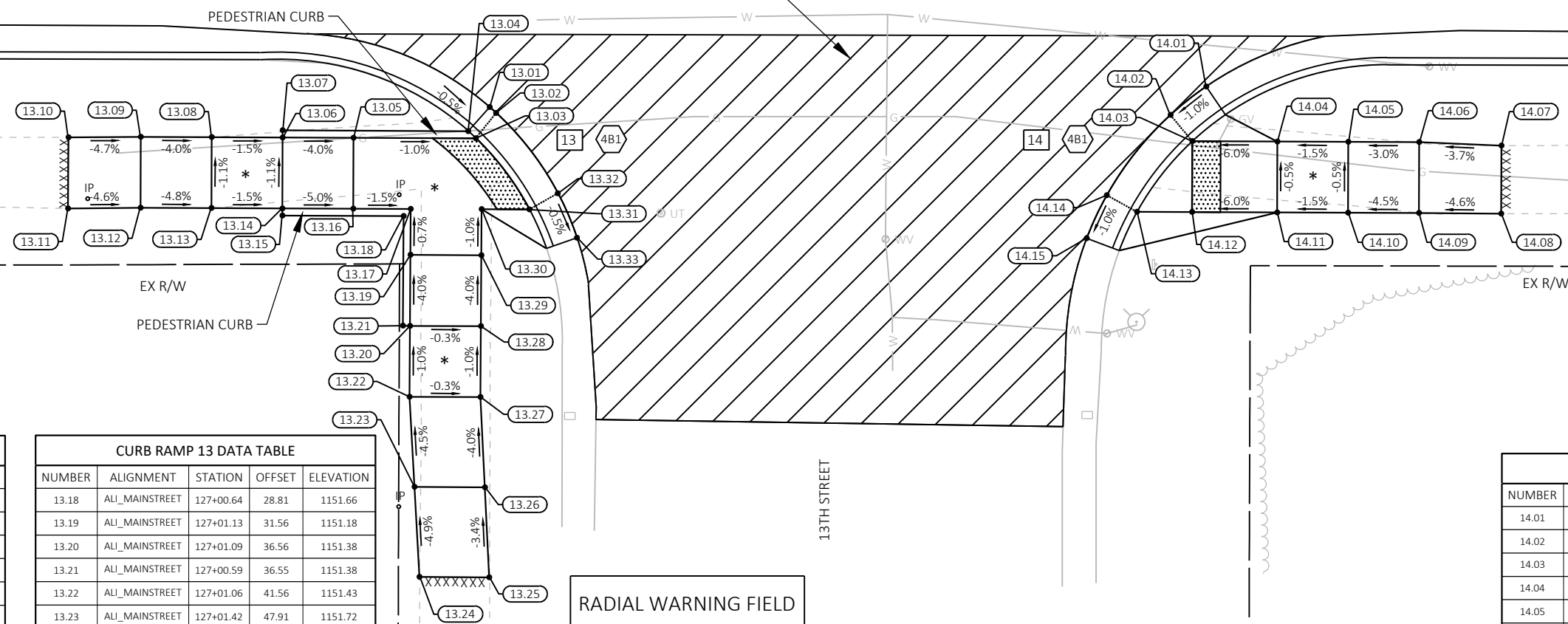
POINT ELEVATIONS AT FACE OF PEDESTRIAN CURB ARE SIDEWALK ELEVATIONS.



126+50 127+00 127+50 128+00

MAIN STREET

PULVERIZE AND RELAY
(DEPTH VARIES, 3" TYPICAL)



CURB RAMP 13 DATA TABLE

NUMBER	ALIGNMENT	STATION	OFFSET	ELEVATION
13.01	ALI_MAINSTREET	127+06.70	21.14	1151.18
13.02	ALI_MAINSTREET	127+07.16	21.54	1151.18
13.03	ALI_MAINSTREET	127+05.73	23.33	1151.13
13.04	ALI_MAINSTREET	127+05.19	22.83	1151.63
13.05	ALI_MAINSTREET	126+97.11	23.30	1151.22
13.06	ALI_MAINSTREET	126+92.11	23.28	1151.42
13.07	ALI_MAINSTREET	126+92.11	22.78	1151.42
13.08	ALI_MAINSTREET	126+87.11	23.26	1151.49
13.09	ALI_MAINSTREET	126+82.11	23.23	1151.69
13.10	ALI_MAINSTREET	126+77.00	23.26	1151.93
13.11	ALI_MAINSTREET	126+76.98	28.28	1152.02
13.12	ALI_MAINSTREET	126+82.09	28.23	1151.78
13.13	ALI_MAINSTREET	126+87.09	28.25	1151.55
13.14	ALI_MAINSTREET	126+92.09	28.28	1151.47
13.15	ALI_MAINSTREET	126+92.09	28.78	1151.47
13.16	ALI_MAINSTREET	126+97.09	28.30	1151.22
13.17	ALI_MAINSTREET	127+01.15	28.31	1151.16

CURB RAMP 13 DATA TABLE

NUMBER	ALIGNMENT	STATION	OFFSET	ELEVATION
13.18	ALI_MAINSTREET	127+00.64	28.81	1151.66
13.19	ALI_MAINSTREET	127+01.13	31.56	1151.18
13.20	ALI_MAINSTREET	127+01.09	36.56	1151.38
13.21	ALI_MAINSTREET	127+00.59	36.55	1151.38
13.22	ALI_MAINSTREET	127+01.06	41.56	1151.43
13.23	ALI_MAINSTREET	127+01.42	47.91	1151.72
13.24	ALI_MAINSTREET	127+01.77	54.27	1152.03
13.25	ALI_MAINSTREET	127+06.68	54.30	1151.89
13.26	ALI_MAINSTREET	127+06.37	47.94	1151.67
13.27	ALI_MAINSTREET	127+06.06	41.59	1151.42
13.28	ALI_MAINSTREET	127+06.09	36.59	1151.37
13.29	ALI_MAINSTREET	127+06.13	31.59	1151.17
13.30	ALI_MAINSTREET	127+06.15	28.34	1151.13
13.31	ALI_MAINSTREET	127+09.51	28.35	1151.10
13.32	ALI_MAINSTREET	127+11.52	27.21	1151.15
13.33	ALI_MAINSTREET	127+12.86	30.34	1151.13

RADIAL WARNING FIELD

LANDING "XR"	12.4'
AREA	13.1 SF
LONG CHORD	6.3'
BOC RADIUS	19.9'

CURB RAMP 14 DATA TABLE

NUMBER	ALIGNMENT	STATION	OFFSET	ELEVATION
14.01	ALI_MAINSTREET	127+57.24	19.69	1151.22
14.02	ALI_MAINSTREET	127+54.72	21.69	1151.19
14.03	ALI_MAINSTREET	127+56.26	23.54	1151.14
14.04	ALI_MAINSTREET	127+62.26	23.56	1151.50
14.05	ALI_MAINSTREET	127+67.26	23.58	1151.57
14.06	ALI_MAINSTREET	127+72.26	23.61	1151.72
14.07	ALI_MAINSTREET	127+78.06	23.84	1151.94
14.08	ALI_MAINSTREET	127+78.04	28.65	1152.09
14.09	ALI_MAINSTREET	127+72.24	28.61	1151.82
14.10	ALI_MAINSTREET	127+67.24	28.58	1151.60
14.11	ALI_MAINSTREET	127+62.24	28.56	1151.52
14.12	ALI_MAINSTREET	127+56.24	28.54	1151.16
14.13	ALI_MAINSTREET	127+52.36	28.52	1151.10
14.14	ALI_MAINSTREET	127+50.23	27.35	1151.15
14.15	ALI_MAINSTREET	127+48.81	30.39	1151.12

LEGEND

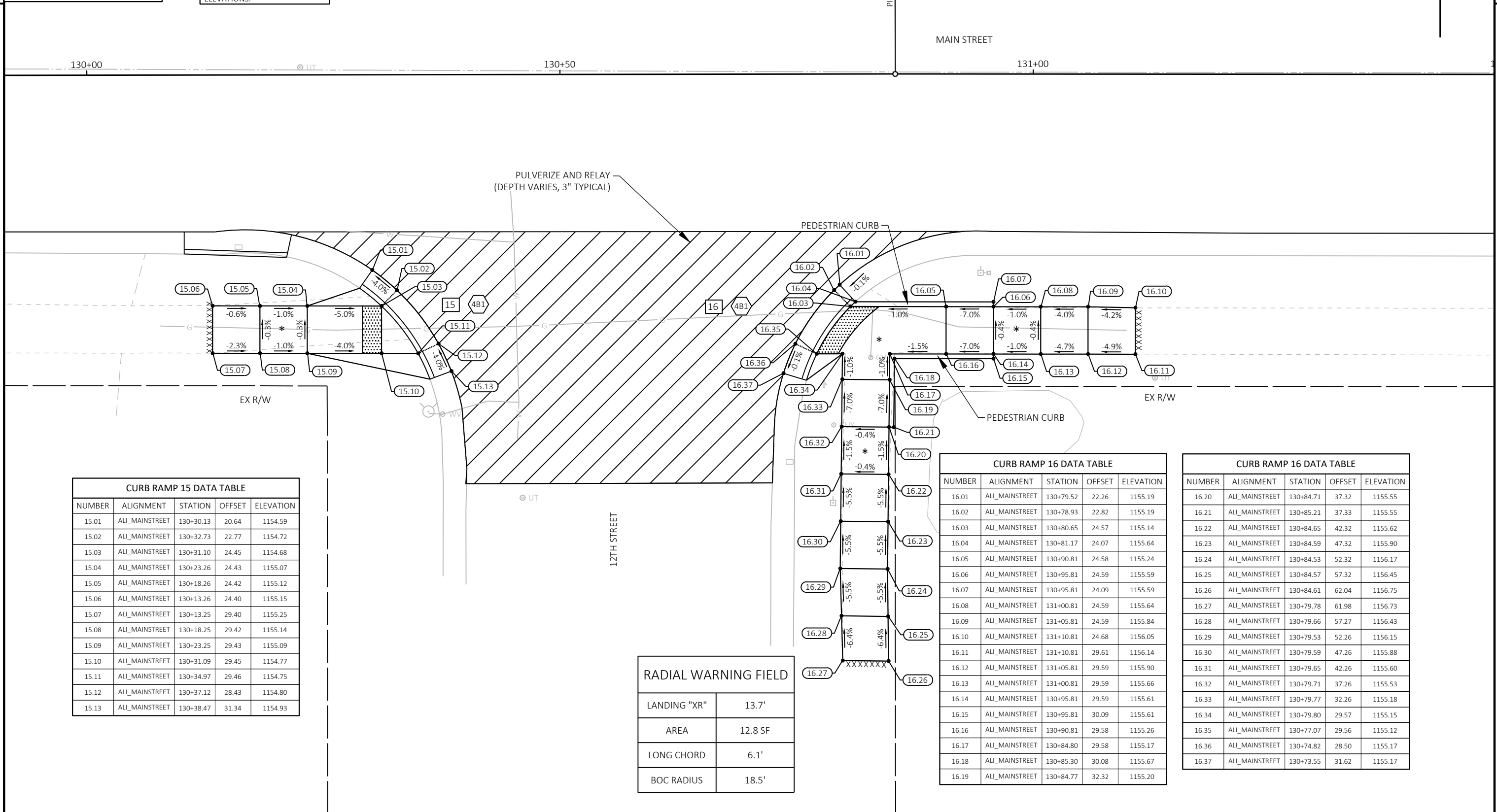
1	CONCRETE CURB RAMP TYPE 1
4A	CONCRETE CURB RAMP TYPE 4A
4B1	CONCRETE CURB RAMP TYPE 4B1
*	LANDING PAD

NOTES:

REFER TO DETAILS "CURB RAMPS" FOR JOINT LOCATIONS AND ADDITIONAL DETAILS.

CURB RAMP TAPERS TO BE GRASS.

POINT ELEVATIONS AT FACE OF PEDESTRIAN CURB ARE SIDEWALK ELEVATIONS.



CURB RAMP 15 DATA TABLE

NUMBER	ALIGNMENT	STATION	OFFSET	ELEVATION
15.01	ALI_MAINSTREET	130+30.13	20.64	1154.59
15.02	ALI_MAINSTREET	130+32.73	22.77	1154.72
15.03	ALI_MAINSTREET	130+31.10	24.45	1154.68
15.04	ALI_MAINSTREET	130+23.26	24.43	1155.07
15.05	ALI_MAINSTREET	130+18.26	24.42	1155.12
15.06	ALI_MAINSTREET	130+13.26	24.40	1155.15
15.07	ALI_MAINSTREET	130+13.25	29.40	1155.25
15.08	ALI_MAINSTREET	130+18.25	29.42	1155.14
15.09	ALI_MAINSTREET	130+23.25	29.43	1155.09
15.10	ALI_MAINSTREET	130+31.09	29.45	1154.77
15.11	ALI_MAINSTREET	130+34.97	29.46	1154.75
15.12	ALI_MAINSTREET	130+37.12	28.43	1154.80
15.13	ALI_MAINSTREET	130+38.47	31.34	1154.93

RADIAL WARNING FIELD

LANDING "XR"	13.7'
AREA	12.8 SF
LONG CHORD	6.1'
BOC RADIUS	18.5'

CURB RAMP 16 DATA TABLE

NUMBER	ALIGNMENT	STATION	OFFSET	ELEVATION
16.01	ALI_MAINSTREET	130+79.52	22.26	1155.19
16.02	ALI_MAINSTREET	130+78.93	22.82	1155.19
16.03	ALI_MAINSTREET	130+80.65	24.57	1155.14
16.04	ALI_MAINSTREET	130+81.17	24.07	1155.64
16.05	ALI_MAINSTREET	130+90.81	24.58	1155.24
16.06	ALI_MAINSTREET	130+95.81	24.59	1155.59
16.07	ALI_MAINSTREET	130+95.81	24.09	1155.59
16.08	ALI_MAINSTREET	131+00.81	24.59	1155.64
16.09	ALI_MAINSTREET	131+05.81	24.59	1155.84
16.10	ALI_MAINSTREET	131+10.81	24.68	1156.05
16.11	ALI_MAINSTREET	131+10.81	29.61	1156.14
16.12	ALI_MAINSTREET	131+05.81	29.59	1155.90
16.13	ALI_MAINSTREET	131+00.81	29.59	1155.66
16.14	ALI_MAINSTREET	130+95.81	29.59	1155.61
16.15	ALI_MAINSTREET	130+95.81	30.09	1155.61
16.16	ALI_MAINSTREET	130+90.81	29.58	1155.26
16.17	ALI_MAINSTREET	130+84.80	29.58	1155.17
16.18	ALI_MAINSTREET	130+85.30	30.08	1155.67
16.19	ALI_MAINSTREET	130+84.77	32.32	1155.20

CURB RAMP 16 DATA TABLE

NUMBER	ALIGNMENT	STATION	OFFSET	ELEVATION
16.20	ALI_MAINSTREET	130+84.71	37.32	1155.55
16.21	ALI_MAINSTREET	130+85.21	37.33	1155.55
16.22	ALI_MAINSTREET	130+84.65	42.32	1155.62
16.23	ALI_MAINSTREET	130+84.59	47.32	1155.90
16.24	ALI_MAINSTREET	130+84.53	52.32	1156.17
16.25	ALI_MAINSTREET	130+84.57	57.32	1156.45
16.26	ALI_MAINSTREET	130+84.61	62.04	1156.75
16.27	ALI_MAINSTREET	130+79.78	61.98	1156.73
16.28	ALI_MAINSTREET	130+79.66	57.27	1156.43
16.29	ALI_MAINSTREET	130+79.53	52.26	1156.15
16.30	ALI_MAINSTREET	130+79.59	47.26	1155.88
16.31	ALI_MAINSTREET	130+79.65	42.26	1155.60
16.32	ALI_MAINSTREET	130+79.71	37.26	1155.53
16.33	ALI_MAINSTREET	130+79.77	32.26	1155.18
16.34	ALI_MAINSTREET	130+79.80	29.57	1155.15
16.35	ALI_MAINSTREET	130+77.07	29.56	1155.12
16.36	ALI_MAINSTREET	130+74.82	28.50	1155.17
16.37	ALI_MAINSTREET	130+73.55	31.62	1155.17

LEGEND

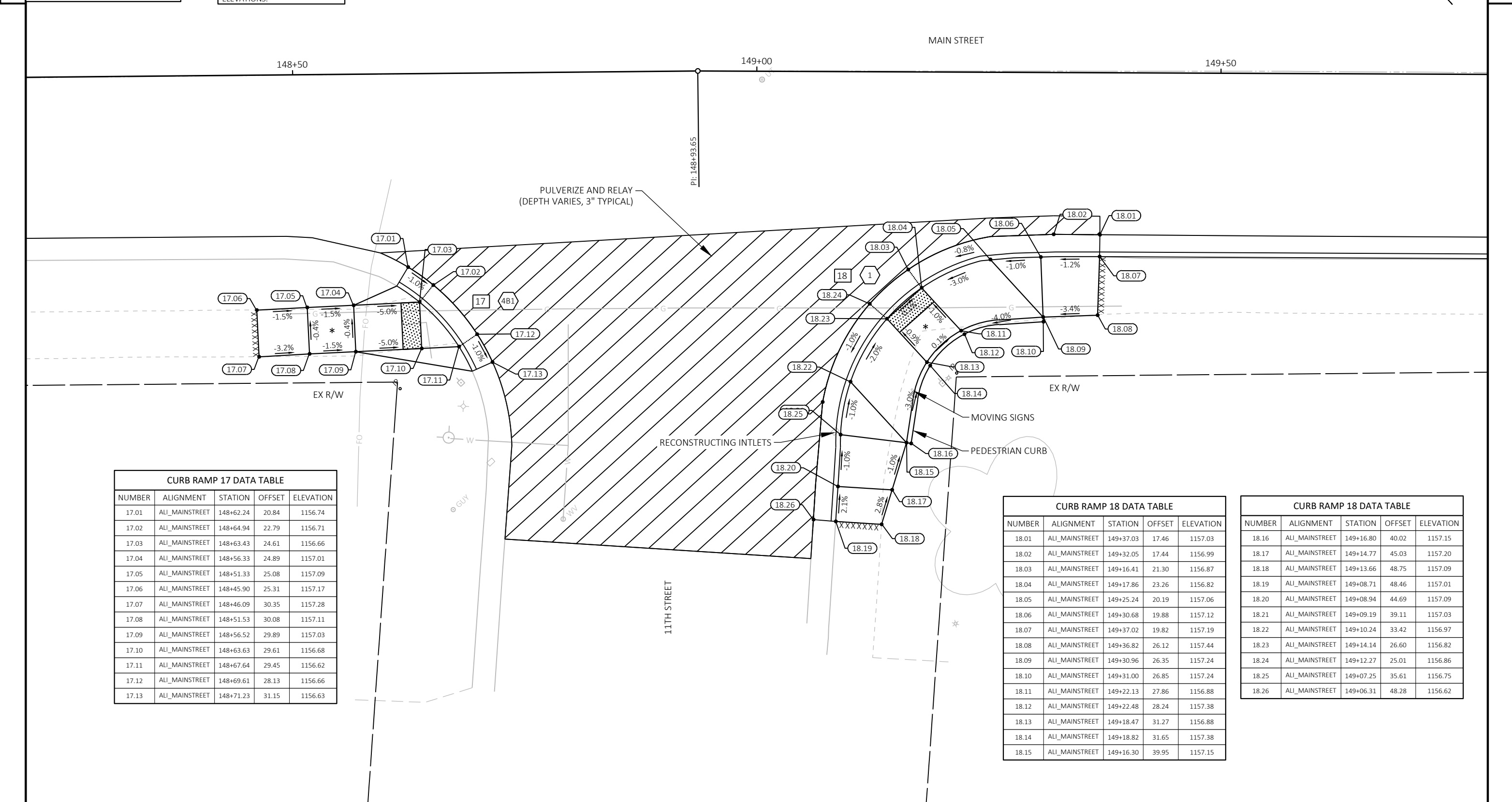
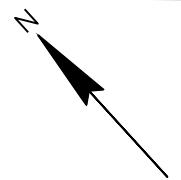
- 1 CONCRETE CURB RAMP TYPE 1
- 4A CONCRETE CURB RAMP TYPE 4A
- 4B1 CONCRETE CURB RAMP TYPE 4B1
- * LANDING PAD

NOTES:

REFER TO DETAILS "CURB RAMPS" FOR JOINT LOCATIONS AND ADDITIONAL DETAILS.

CURB RAMP TAPERS TO BE GRASS.

POINT ELEVATIONS AT FACE OF PEDESTRIAN CURB ARE SIDEWALK ELEVATIONS.



CURB RAMP 17 DATA TABLE

NUMBER	ALIGNMENT	STATION	OFFSET	ELEVATION
17.01	ALI_MAINSTREET	148+62.24	20.84	1156.74
17.02	ALI_MAINSTREET	148+64.94	22.79	1156.71
17.03	ALI_MAINSTREET	148+63.43	24.61	1156.66
17.04	ALI_MAINSTREET	148+56.33	24.89	1157.01
17.05	ALI_MAINSTREET	148+51.33	25.08	1157.09
17.06	ALI_MAINSTREET	148+45.90	25.31	1157.17
17.07	ALI_MAINSTREET	148+46.09	30.35	1157.28
17.08	ALI_MAINSTREET	148+51.53	30.08	1157.11
17.09	ALI_MAINSTREET	148+56.52	29.89	1157.03
17.10	ALI_MAINSTREET	148+63.63	29.61	1156.68
17.11	ALI_MAINSTREET	148+67.64	29.45	1156.62
17.12	ALI_MAINSTREET	148+69.61	28.13	1156.66
17.13	ALI_MAINSTREET	148+71.23	31.15	1156.63

CURB RAMP 18 DATA TABLE

NUMBER	ALIGNMENT	STATION	OFFSET	ELEVATION
18.01	ALI_MAINSTREET	149+37.03	17.46	1157.03
18.02	ALI_MAINSTREET	149+32.05	17.44	1156.99
18.03	ALI_MAINSTREET	149+16.41	21.30	1156.87
18.04	ALI_MAINSTREET	149+17.86	23.26	1156.82
18.05	ALI_MAINSTREET	149+25.24	20.19	1157.06
18.06	ALI_MAINSTREET	149+30.68	19.88	1157.12
18.07	ALI_MAINSTREET	149+37.02	19.82	1157.19
18.08	ALI_MAINSTREET	149+36.82	26.12	1157.44
18.09	ALI_MAINSTREET	149+30.96	26.35	1157.24
18.10	ALI_MAINSTREET	149+31.00	26.85	1157.24
18.11	ALI_MAINSTREET	149+22.13	27.86	1156.88
18.12	ALI_MAINSTREET	149+22.48	28.24	1157.38
18.13	ALI_MAINSTREET	149+18.47	31.27	1156.88
18.14	ALI_MAINSTREET	149+18.82	31.65	1157.38
18.15	ALI_MAINSTREET	149+16.30	39.95	1157.15

CURB RAMP 18 DATA TABLE

NUMBER	ALIGNMENT	STATION	OFFSET	ELEVATION
18.16	ALI_MAINSTREET	149+16.80	40.02	1157.15
18.17	ALI_MAINSTREET	149+14.77	45.03	1157.20
18.18	ALI_MAINSTREET	149+13.66	48.75	1157.09
18.19	ALI_MAINSTREET	149+08.71	48.46	1157.01
18.20	ALI_MAINSTREET	149+08.94	44.69	1157.09
18.21	ALI_MAINSTREET	149+09.19	39.11	1157.03
18.22	ALI_MAINSTREET	149+10.24	33.42	1156.97
18.23	ALI_MAINSTREET	149+14.14	26.60	1156.82
18.24	ALI_MAINSTREET	149+12.27	25.01	1156.86
18.25	ALI_MAINSTREET	149+07.25	35.61	1156.75
18.26	ALI_MAINSTREET	149+06.31	48.28	1156.62

LEGEND

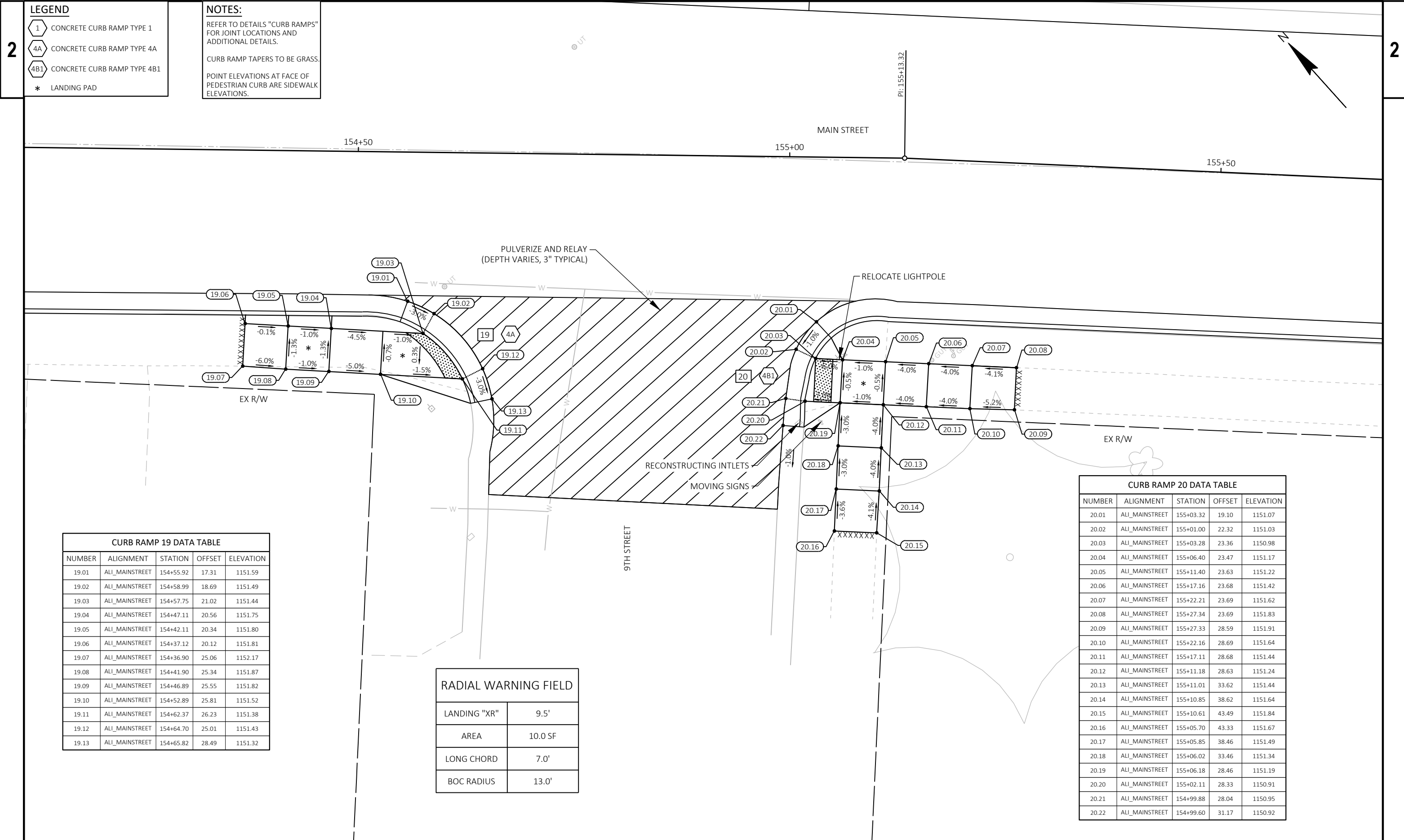
1	CONCRETE CURB RAMP TYPE 1
4A	CONCRETE CURB RAMP TYPE 4A
4B1	CONCRETE CURB RAMP TYPE 4B1
*	LANDING PAD

NOTES:

REFER TO DETAILS "CURB RAMPS" FOR JOINT LOCATIONS AND ADDITIONAL DETAILS.

CURB RAMP TAPERS TO BE GRASS.

POINT ELEVATIONS AT FACE OF PEDESTRIAN CURB ARE SIDEWALK ELEVATIONS.



CURB RAMP 19 DATA TABLE

NUMBER	ALIGNMENT	STATION	OFFSET	ELEVATION
19.01	ALI_MAINSTREET	154+55.92	17.31	1151.59
19.02	ALI_MAINSTREET	154+58.99	18.69	1151.49
19.03	ALI_MAINSTREET	154+57.75	21.02	1151.44
19.04	ALI_MAINSTREET	154+47.11	20.56	1151.75
19.05	ALI_MAINSTREET	154+42.11	20.34	1151.80
19.06	ALI_MAINSTREET	154+37.12	20.12	1151.81
19.07	ALI_MAINSTREET	154+36.90	25.06	1152.17
19.08	ALI_MAINSTREET	154+41.90	25.34	1151.87
19.09	ALI_MAINSTREET	154+46.89	25.55	1151.82
19.10	ALI_MAINSTREET	154+52.89	25.81	1151.52
19.11	ALI_MAINSTREET	154+62.37	26.23	1151.38
19.12	ALI_MAINSTREET	154+64.70	25.01	1151.43
19.13	ALI_MAINSTREET	154+65.82	28.49	1151.32

RADIAL WARNING FIELD

LANDING "XR"	9.5'
AREA	10.0 SF
LONG CHORD	7.0'
BOC RADIUS	13.0'

CURB RAMP 20 DATA TABLE

NUMBER	ALIGNMENT	STATION	OFFSET	ELEVATION
20.01	ALI_MAINSTREET	155+03.32	19.10	1151.07
20.02	ALI_MAINSTREET	155+01.00	22.32	1151.03
20.03	ALI_MAINSTREET	155+03.28	23.36	1150.98
20.04	ALI_MAINSTREET	155+06.40	23.47	1151.17
20.05	ALI_MAINSTREET	155+11.40	23.63	1151.22
20.06	ALI_MAINSTREET	155+17.16	23.68	1151.42
20.07	ALI_MAINSTREET	155+22.21	23.69	1151.62
20.08	ALI_MAINSTREET	155+27.34	23.69	1151.83
20.09	ALI_MAINSTREET	155+27.33	28.59	1151.91
20.10	ALI_MAINSTREET	155+22.16	28.69	1151.64
20.11	ALI_MAINSTREET	155+17.11	28.68	1151.44
20.12	ALI_MAINSTREET	155+11.18	28.63	1151.24
20.13	ALI_MAINSTREET	155+11.01	33.62	1151.44
20.14	ALI_MAINSTREET	155+10.85	38.62	1151.64
20.15	ALI_MAINSTREET	155+10.61	43.49	1151.84
20.16	ALI_MAINSTREET	155+05.70	43.33	1151.67
20.17	ALI_MAINSTREET	155+05.85	38.46	1151.49
20.18	ALI_MAINSTREET	155+06.02	33.46	1151.34
20.19	ALI_MAINSTREET	155+06.18	28.46	1151.19
20.20	ALI_MAINSTREET	155+02.11	28.33	1150.91
20.21	ALI_MAINSTREET	154+99.88	28.04	1150.95
20.22	ALI_MAINSTREET	154+99.60	31.17	1150.92

LEGEND

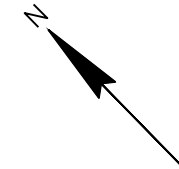
- 1 CONCRETE CURB RAMP TYPE 1
- 4A CONCRETE CURB RAMP TYPE 4A
- 4B1 CONCRETE CURB RAMP TYPE 4B1
- * LANDING PAD

NOTES:

REFER TO DETAILS "CURB RAMP" FOR JOINT LOCATIONS AND ADDITIONAL DETAILS.

CURB RAMP TAPERS TO BE GRASS.

POINT ELEVATIONS AT FACE OF PEDESTRIAN CURB ARE SIDEWALK ELEVATIONS.



2

2

157+50

158+00

158+50

MAIN STREET

PULVERIZE AND RELAY
(DEPTH VARIES, 3" TYPICAL)

FLATTEN GUTTER FLAG SLOPE THROUGH RAMP

MOVING SIGNS

8TH STREET

EX R/W

EX R/W

CURB RAMP 21 DATA TABLE

NUMBER	ALIGNMENT	STATION	OFFSET	ELEVATION
21.01	ALI_MAINSTREET	157+85.23	19.05	1152.82
21.02	ALI_MAINSTREET	157+86.85	23.66	1152.86
21.03	ALI_MAINSTREET	157+84.59	23.73	1152.81
21.04	ALI_MAINSTREET	157+79.79	23.73	1152.99
21.05	ALI_MAINSTREET	157+74.79	23.72	1153.17
21.06	ALI_MAINSTREET	157+69.49	23.73	1153.12
21.07	ALI_MAINSTREET	157+69.49	28.60	1153.18
21.08	ALI_MAINSTREET	157+74.78	28.72	1153.19
21.09	ALI_MAINSTREET	157+74.85	34.76	1153.22
21.10	ALI_MAINSTREET	157+79.77	34.76	1153.15
21.11	ALI_MAINSTREET	157+79.78	28.73	1153.14
21.12	ALI_MAINSTREET	157+84.76	28.73	1152.84
21.13	ALI_MAINSTREET	157+87.15	28.66	1152.89
21.14	ALI_MAINSTREET	157+87.15	31.77	1152.91

CURB RAMP 22 DATA TABLE

NUMBER	ALIGNMENT	STATION	OFFSET	ELEVATION
22.01	ALI_MAINSTREET	158+23.97	19.59	1152.90
22.02	ALI_MAINSTREET	158+21.77	22.63	1152.98
22.03	ALI_MAINSTREET	158+23.70	23.72	1152.96
22.04	ALI_MAINSTREET	158+28.32	23.72	1153.31
22.05	ALI_MAINSTREET	158+33.32	23.72	1153.39
22.06	ALI_MAINSTREET	158+37.25	23.85	1153.50
22.07	ALI_MAINSTREET	158+37.26	28.57	1153.50
22.08	ALI_MAINSTREET	158+33.32	28.72	1153.46
22.09	ALI_MAINSTREET	158+28.32	28.72	1153.39
22.10	ALI_MAINSTREET	158+22.85	28.72	1153.00
22.11	ALI_MAINSTREET	158+20.50	28.71	1153.02
22.12	ALI_MAINSTREET	158+20.42	31.70	1153.11

LEGEND

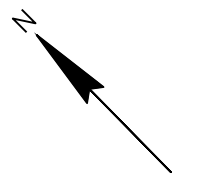
1	CONCRETE CURB RAMP TYPE 1
4A	CONCRETE CURB RAMP TYPE 4A
4B1	CONCRETE CURB RAMP TYPE 4B1
*	LANDING PAD

NOTES:

REFER TO DETAILS "CURB RAMPS" FOR JOINT LOCATIONS AND ADDITIONAL DETAILS.

CURB RAMP TAPERS TO BE GRASS.

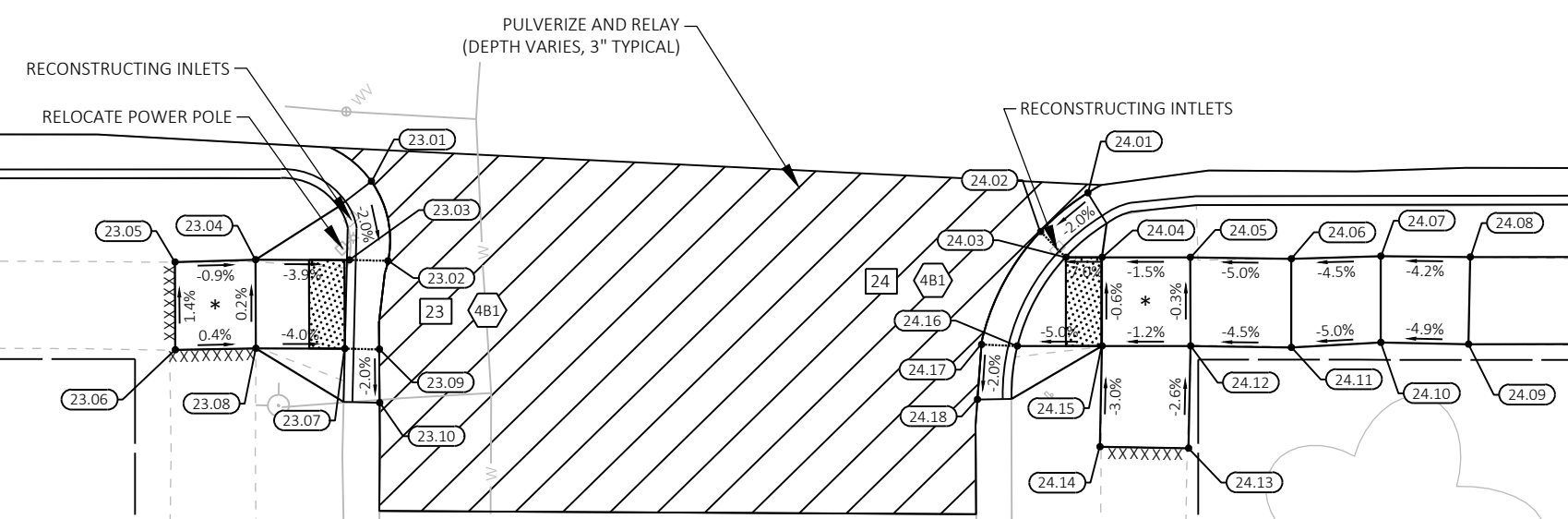
POINT ELEVATIONS AT FACE OF PEDESTRIAN CURB ARE SIDEWALK ELEVATIONS.



2

2

161+00 161+50 MAIN STREET 162+00



CURB RAMP 23 DATA TABLE

NUMBER	ALIGNMENT	STATION	OFFSET	ELEVATION
23.01	ALI_MAINSTREET	161+06.62	19.39	1155.31
23.02	ALI_MAINSTREET	161+07.57	23.86	1155.22
23.03	ALI_MAINSTREET	161+05.37	23.81	1155.17
23.04	ALI_MAINSTREET	161+00.10	23.80	1155.38
23.05	ALI_MAINSTREET	160+95.54	23.93	1155.42
23.06	ALI_MAINSTREET	160+95.56	28.89	1155.35
23.07	ALI_MAINSTREET	161+05.12	28.81	1155.17
23.08	ALI_MAINSTREET	161+00.11	28.80	1155.37
23.09	ALI_MAINSTREET	161+07.07	28.86	1155.21
23.10	ALI_MAINSTREET	161+07.09	31.87	1155.15

CURB RAMP 24 DATA TABLE

NUMBER	ALIGNMENT	STATION	OFFSET	ELEVATION
24.01	ALI_MAINSTREET	161+47.06	20.03	1156.05
24.02	ALI_MAINSTREET	161+44.39	22.22	1155.98
24.03	ALI_MAINSTREET	161+45.83	23.67	1155.94
24.04	ALI_MAINSTREET	161+47.85	23.67	1156.08
24.05	ALI_MAINSTREET	161+52.85	23.67	1156.16
24.06	ALI_MAINSTREET	161+58.54	23.74	1156.44
24.07	ALI_MAINSTREET	161+63.58	23.61	1156.67
24.08	ALI_MAINSTREET	161+68.64	23.65	1156.88
24.09	ALI_MAINSTREET	161+68.54	28.56	1156.92
24.10	ALI_MAINSTREET	161+63.59	28.46	1156.68
24.11	ALI_MAINSTREET	161+58.54	28.74	1156.43
24.12	ALI_MAINSTREET	161+52.85	28.67	1156.17
24.13	ALI_MAINSTREET	161+52.74	34.44	1156.32
24.14	ALI_MAINSTREET	161+47.74	34.34	1156.28
24.15	ALI_MAINSTREET	161+47.86	28.67	1156.11
24.16	ALI_MAINSTREET	161+43.08	28.67	1155.87
24.17	ALI_MAINSTREET	161+41.05	28.58	1155.91
24.18	ALI_MAINSTREET	161+40.83	31.68	1155.85

LEGEND

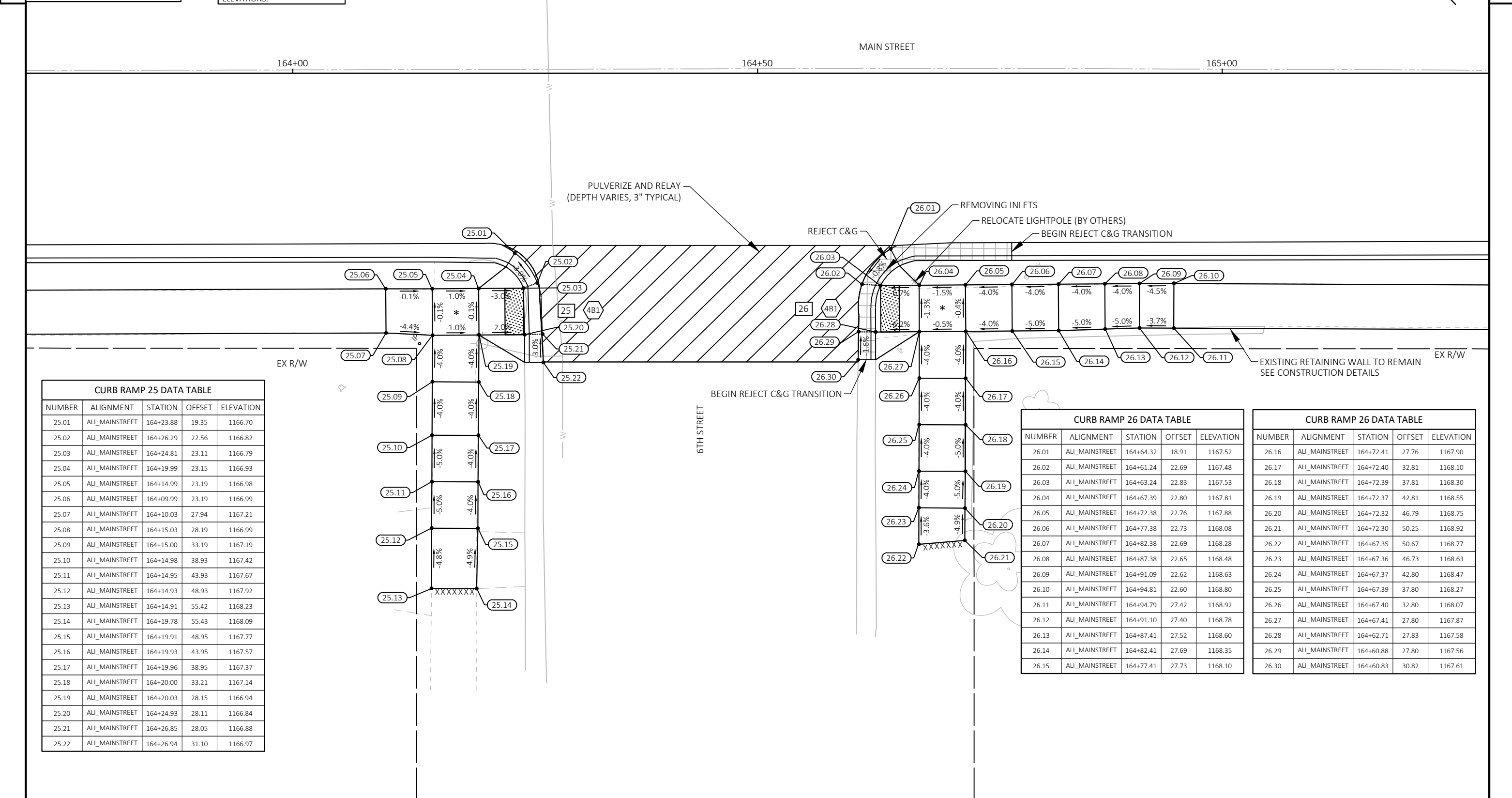
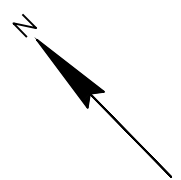
1	CONCRETE CURB RAMP TYPE 1
4A	CONCRETE CURB RAMP TYPE 4A
4B1	CONCRETE CURB RAMP TYPE 4B1
*	LANDING PAD

NOTES:

REFER TO DETAILS "CURB RAMP" FOR JOINT LOCATIONS AND ADDITIONAL DETAILS.

CURB RAMP TAPERS TO BE GRASS.

POINT ELEVATIONS AT FACE OF PEDESTRIAN CURB ARE SIDEWALK ELEVATIONS.



CURB RAMP 25 DATA TABLE

NUMBER	ALIGNMENT	STATION	OFFSET	ELEVATION
25.01	ALI_MAINSTREET	164+23.88	19.35	1166.70
25.02	ALI_MAINSTREET	164+26.29	22.56	1166.82
25.03	ALI_MAINSTREET	164+24.81	23.11	1166.79
25.04	ALI_MAINSTREET	164+19.99	23.15	1166.93
25.05	ALI_MAINSTREET	164+14.99	23.19	1166.98
25.06	ALI_MAINSTREET	164+09.99	23.19	1166.99
25.07	ALI_MAINSTREET	164+10.03	27.94	1167.21
25.08	ALI_MAINSTREET	164+15.03	28.19	1166.99
25.09	ALI_MAINSTREET	164+15.00	33.19	1167.19
25.10	ALI_MAINSTREET	164+14.98	38.93	1167.42
25.11	ALI_MAINSTREET	164+14.95	43.93	1167.67
25.12	ALI_MAINSTREET	164+14.93	48.93	1167.92
25.13	ALI_MAINSTREET	164+14.91	55.42	1168.23
25.14	ALI_MAINSTREET	164+19.78	55.43	1168.09
25.15	ALI_MAINSTREET	164+19.91	48.95	1167.77
25.16	ALI_MAINSTREET	164+19.93	43.95	1167.57
25.17	ALI_MAINSTREET	164+19.96	38.95	1167.37
25.18	ALI_MAINSTREET	164+20.00	33.21	1167.14
25.19	ALI_MAINSTREET	164+20.03	28.15	1166.94
25.20	ALI_MAINSTREET	164+24.93	28.11	1166.84
25.21	ALI_MAINSTREET	164+26.85	28.05	1166.88
25.22	ALI_MAINSTREET	164+26.94	31.10	1166.97

CURB RAMP 26 DATA TABLE

NUMBER	ALIGNMENT	STATION	OFFSET	ELEVATION
26.01	ALI_MAINSTREET	164+64.32	18.91	1167.52
26.02	ALI_MAINSTREET	164+61.24	22.69	1167.48
26.03	ALI_MAINSTREET	164+63.24	22.83	1167.53
26.04	ALI_MAINSTREET	164+67.39	22.80	1167.81
26.05	ALI_MAINSTREET	164+72.38	22.76	1167.88
26.06	ALI_MAINSTREET	164+77.38	22.73	1168.08
26.07	ALI_MAINSTREET	164+82.38	22.69	1168.28
26.08	ALI_MAINSTREET	164+87.38	22.65	1168.48
26.09	ALI_MAINSTREET	164+91.09	22.62	1168.63
26.10	ALI_MAINSTREET	164+94.81	22.60	1168.80
26.11	ALI_MAINSTREET	164+94.79	27.42	1168.92
26.12	ALI_MAINSTREET	164+91.10	27.40	1168.78
26.13	ALI_MAINSTREET	164+87.41	27.52	1168.60
26.14	ALI_MAINSTREET	164+82.41	27.69	1168.35
26.15	ALI_MAINSTREET	164+77.41	27.73	1168.10

CURB RAMP 26 DATA TABLE

NUMBER	ALIGNMENT	STATION	OFFSET	ELEVATION
26.16	ALI_MAINSTREET	164+72.41	27.76	1167.90
26.17	ALI_MAINSTREET	164+72.40	32.81	1168.10
26.18	ALI_MAINSTREET	164+72.39	37.81	1168.30
26.19	ALI_MAINSTREET	164+72.37	42.81	1168.55
26.20	ALI_MAINSTREET	164+72.32	46.79	1168.75
26.21	ALI_MAINSTREET	164+72.30	50.25	1168.92
26.22	ALI_MAINSTREET	164+67.35	50.67	1168.77
26.23	ALI_MAINSTREET	164+67.36	46.73	1168.63
26.24	ALI_MAINSTREET	164+67.37	42.80	1168.47
26.25	ALI_MAINSTREET	164+67.39	37.80	1168.27
26.26	ALI_MAINSTREET	164+67.40	32.80	1168.07
26.27	ALI_MAINSTREET	164+67.41	27.80	1167.87
26.28	ALI_MAINSTREET	164+62.71	27.83	1167.58
26.29	ALI_MAINSTREET	164+60.88	27.80	1167.56
26.30	ALI_MAINSTREET	164+60.83	30.82	1167.61

LEGEND

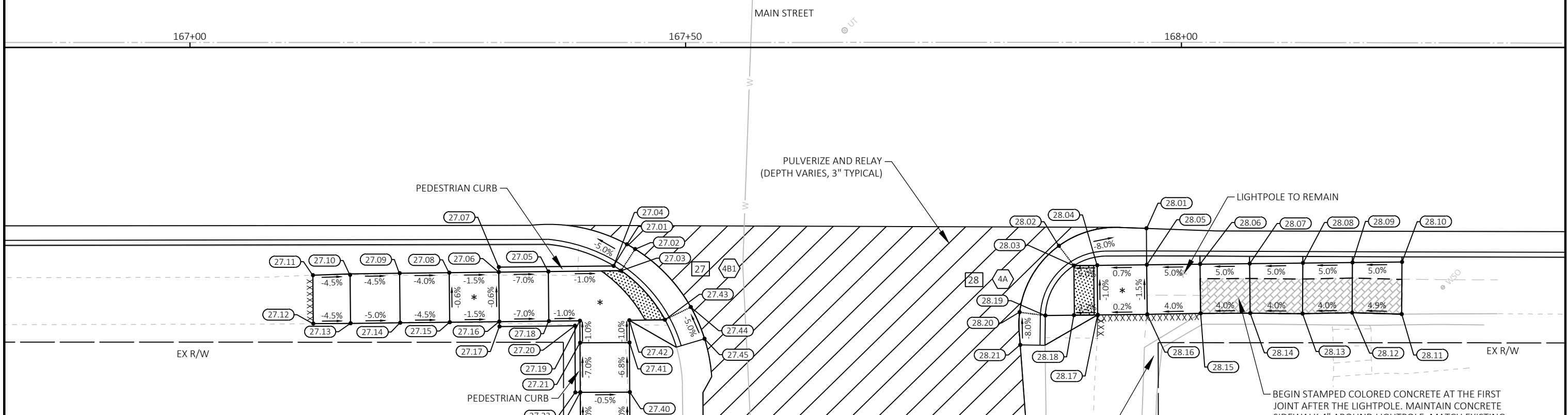
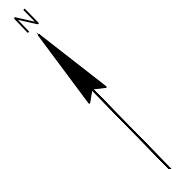
1	CONCRETE CURB RAMP TYPE 1
4A	CONCRETE CURB RAMP TYPE 4A
4B1	CONCRETE CURB RAMP TYPE 4B1
*	LANDING PAD

NOTES:

REFER TO DETAILS "CURB RAMPS" FOR JOINT LOCATIONS AND ADDITIONAL DETAILS.

CURB RAMP TAPERS TO BE GRASS.

POINT ELEVATIONS AT FACE OF PEDESTRIAN CURB ARE SIDEWALK ELEVATIONS.

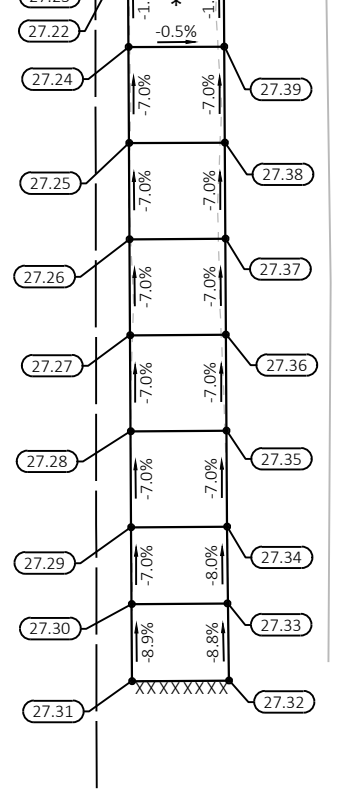


CURB RAMP 27 DATA TABLE

NUMBER	ALIGNMENT	STATION	OFFSET	ELEVATION
27.01	ALI_MAINSTREET	167+44.06	19.87	1169.02
27.02	ALI_MAINSTREET	167+44.90	20.36	1169.07
27.03	ALI_MAINSTREET	167+43.47	22.53	1169.02
27.04	ALI_MAINSTREET	167+42.72	22.04	1169.52
27.05	ALI_MAINSTREET	167+36.14	22.62	1169.09
27.06	ALI_MAINSTREET	167+31.14	22.67	1169.44
27.07	ALI_MAINSTREET	167+31.13	22.17	1169.44
27.08	ALI_MAINSTREET	167+26.14	22.73	1169.52
27.09	ALI_MAINSTREET	167+21.14	22.79	1169.72
27.10	ALI_MAINSTREET	167+16.14	22.95	1169.94
27.11	ALI_MAINSTREET	167+12.39	22.98	1170.11
27.12	ALI_MAINSTREET	167+12.43	27.87	1170.19
27.13	ALI_MAINSTREET	167+16.20	27.84	1170.02
27.14	ALI_MAINSTREET	167+21.20	27.79	1169.77
27.15	ALI_MAINSTREET	167+26.20	27.73	1169.55
27.16	ALI_MAINSTREET	167+31.20	27.67	1169.47
27.17	ALI_MAINSTREET	167+31.20	28.17	1169.42
27.18	ALI_MAINSTREET	167+36.20	27.62	1169.12
27.19	ALI_MAINSTREET	167+39.33	27.58	1169.09
27.20	ALI_MAINSTREET	167+38.83	28.09	1169.59
27.21	ALI_MAINSTREET	167+39.34	29.81	1169.11
27.22	ALI_MAINSTREET	167+39.36	34.81	1169.46
27.23	ALI_MAINSTREET	167+38.86	34.81	1169.46

CURB RAMP 27 DATA TABLE

NUMBER	ALIGNMENT	STATION	OFFSET	ELEVATION
27.24	ALI_MAINSTREET	167+39.39	39.81	1169.51
27.25	ALI_MAINSTREET	167+39.42	44.81	1169.86
27.26	ALI_MAINSTREET	167+39.45	49.81	1170.21
27.27	ALI_MAINSTREET	167+39.48	54.81	1170.56
27.28	ALI_MAINSTREET	167+39.50	59.81	1170.91
27.29	ALI_MAINSTREET	167+39.53	64.81	1171.26
27.30	ALI_MAINSTREET	167+39.55	68.81	1171.54
27.31	ALI_MAINSTREET	167+39.57	72.82	1171.90
27.32	ALI_MAINSTREET	167+44.63	72.79	1171.91
27.33	ALI_MAINSTREET	167+44.54	68.79	1171.56
27.34	ALI_MAINSTREET	167+44.52	64.78	1171.24
27.35	ALI_MAINSTREET	167+44.49	59.78	1170.89
27.36	ALI_MAINSTREET	167+44.46	54.78	1170.54
27.37	ALI_MAINSTREET	167+44.43	49.78	1170.19
27.38	ALI_MAINSTREET	167+44.41	44.78	1169.84
27.39	ALI_MAINSTREET	167+44.38	39.78	1169.49
27.40	ALI_MAINSTREET	167+44.35	34.78	1169.44
27.41	ALI_MAINSTREET	167+44.32	29.78	1169.10
27.42	ALI_MAINSTREET	167+44.31	27.53	1169.08
27.43	ALI_MAINSTREET	167+47.90	27.49	1169.07
27.44	ALI_MAINSTREET	167+50.50	26.21	1169.13
27.45	ALI_MAINSTREET	167+51.89	29.40	1169.30



RADIAL WARNING FIELD

LANDING "XR"	11.7'
AREA	10.0 SF
LONG CHORD	6.6'
BOC RADIUS	14.5'

CURB RAMP 28 DATA TABLE

NUMBER	ALIGNMENT	STATION	OFFSET	ELEVATION
28.01	ALI_MAINSTREET	167+96.48	18.26	1167.67
28.02	ALI_MAINSTREET	167+87.64	20.02	1168.39
28.03	ALI_MAINSTREET	167+89.16	22.02	1168.34
28.04	ALI_MAINSTREET	167+91.50	21.99	1168.46
28.05	ALI_MAINSTREET	167+96.50	21.94	1168.42
28.06	ALI_MAINSTREET	168+01.89	21.87	1168.15
28.07	ALI_MAINSTREET	168+06.89	21.82	1167.90
28.08	ALI_MAINSTREET	168+12.26	21.76	1167.63
28.09	ALI_MAINSTREET	168+17.26	21.70	1167.38
28.10	ALI_MAINSTREET	168+22.26	21.64	1167.13
28.11	ALI_MAINSTREET	168+22.22	26.91	1167.43
28.12	ALI_MAINSTREET	168+17.22	26.77	1167.67
28.13	ALI_MAINSTREET	168+12.22	26.87	1167.87
28.14	ALI_MAINSTREET	168+06.93	26.82	1168.08
28.15	ALI_MAINSTREET	168+01.93	26.84	1168.28
28.16	ALI_MAINSTREET	167+96.56	26.94	1168.50
28.17	ALI_MAINSTREET	167+91.56	26.99	1168.51
28.18	ALI_MAINSTREET	167+89.16	27.02	1168.46
28.19	ALI_MAINSTREET	167+86.28	27.05	1168.39
28.20	ALI_MAINSTREET	167+83.67	26.72	1168.45
28.21	ALI_MAINSTREET	167+83.74	30.02	1168.71

LEGEND

- 1 CONCRETE CURB RAMP TYPE 1
- 4A CONCRETE CURB RAMP TYPE 4A
- 4B1 CONCRETE CURB RAMP TYPE 4B1
- * LANDING PAD

NOTES:

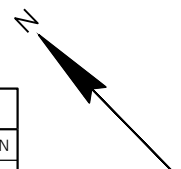
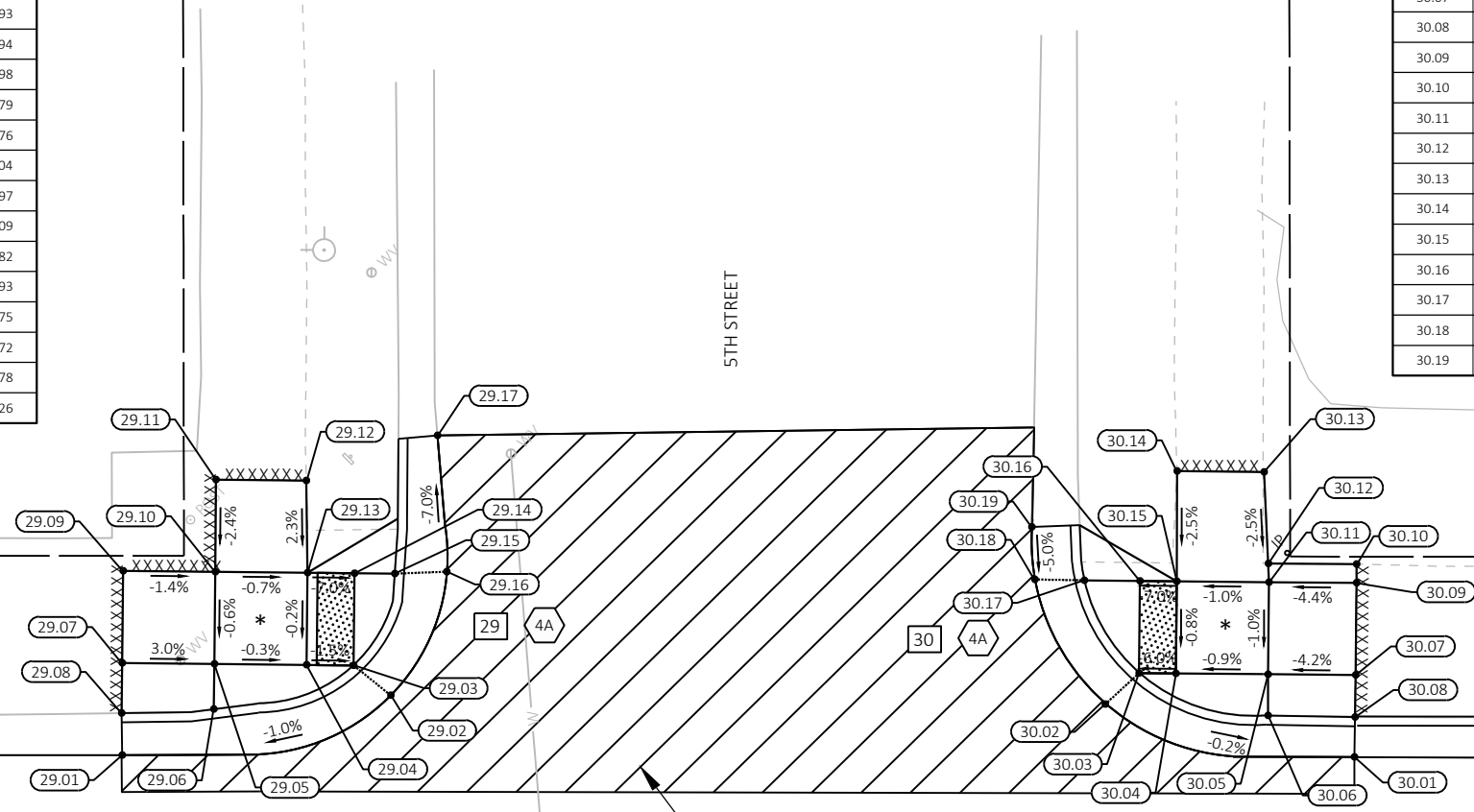
REFER TO DETAILS "CURB RAMPS" FOR JOINT LOCATIONS AND ADDITIONAL DETAILS.

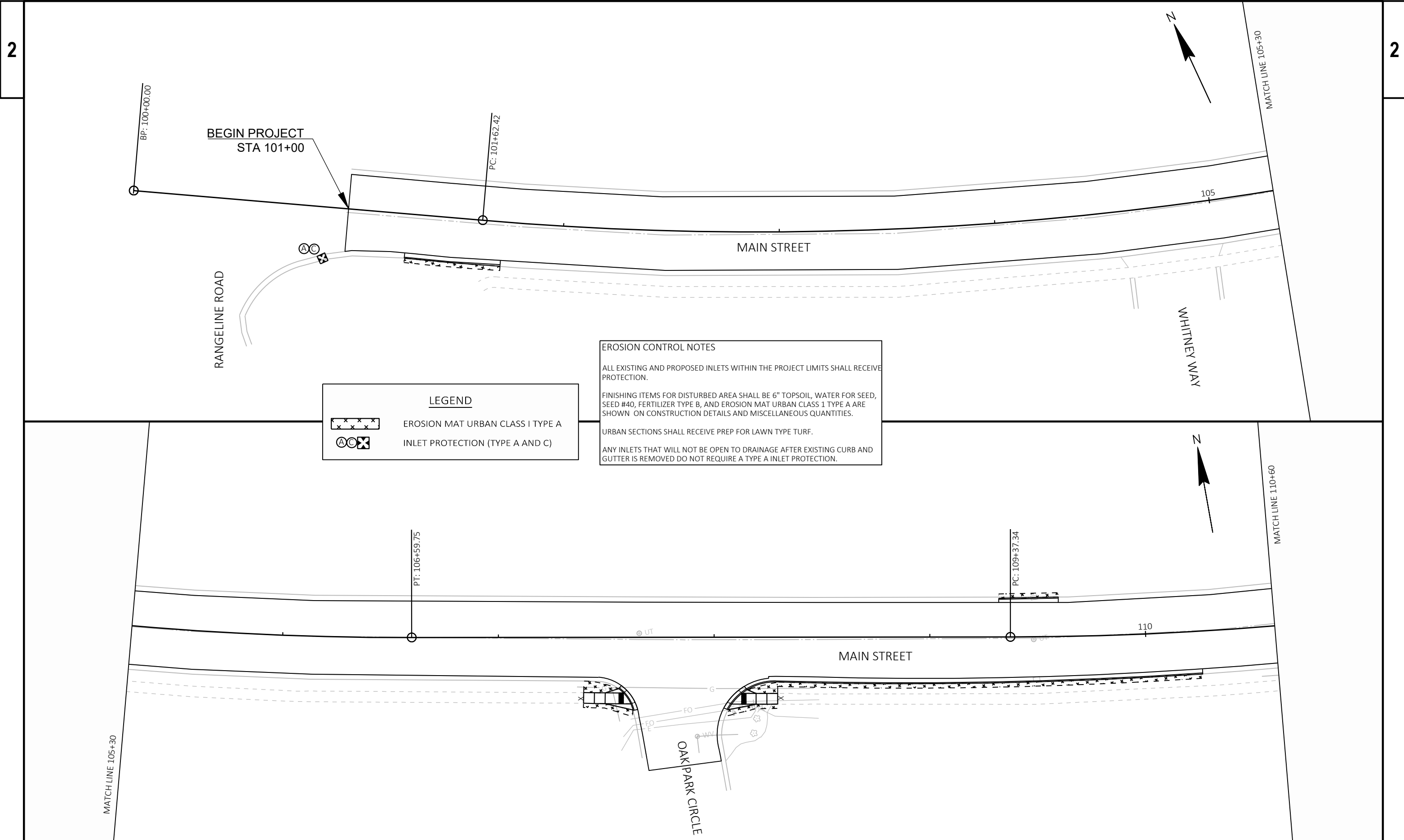
CURB RAMP TAPERS TO BE GRASS.

POINT ELEVATIONS AT FACE OF PEDESTRIAN CURB ARE SIDEWALK ELEVATIONS.

CURB RAMP 29 DATA TABLE				
NUMBER	ALIGNMENT	STATION	OFFSET	ELEVATION
29.01	ALI_MAINSTREET	167+34.22	-19.43	1168.79
29.02	ALI_MAINSTREET	167+48.79	-22.68	1168.94
29.03	ALI_MAINSTREET	167+46.77	-24.31	1168.89
29.04	ALI_MAINSTREET	167+44.22	-24.33	1168.93
29.05	ALI_MAINSTREET	167+39.22	-24.38	1168.94
29.06	ALI_MAINSTREET	167+39.20	-21.93	1168.98
29.07	ALI_MAINSTREET	167+34.22	-24.43	1168.79
29.08	ALI_MAINSTREET	167+34.19	-21.72	1168.76
29.09	ALI_MAINSTREET	167+34.27	-29.43	1169.04
29.10	ALI_MAINSTREET	167+39.27	-29.38	1168.97
29.11	ALI_MAINSTREET	167+39.32	-34.38	1169.09
29.12	ALI_MAINSTREET	167+44.21	-34.33	1168.82
29.13	ALI_MAINSTREET	167+44.27	-29.33	1168.93
29.14	ALI_MAINSTREET	167+46.82	-29.31	1168.75
29.15	ALI_MAINSTREET	167+49.01	-29.28	1168.72
29.16	ALI_MAINSTREET	167+51.83	-29.39	1168.78
29.17	ALI_MAINSTREET	167+51.33	-36.78	1168.26

CURB RAMP 30 DATA TABLE				
NUMBER	ALIGNMENT	STATION	OFFSET	ELEVATION
30.01	ALI_MAINSTREET	168+01.06	-19.33	1167.40
30.02	ALI_MAINSTREET	167+87.55	-22.19	1167.43
30.03	ALI_MAINSTREET	167+89.38	-23.89	1167.38
30.04	ALI_MAINSTREET	167+91.38	-23.87	1167.50
30.05	ALI_MAINSTREET	167+96.38	-23.82	1167.54
30.06	ALI_MAINSTREET	167+96.38	-21.59	1167.66
30.07	ALI_MAINSTREET	168+01.12	-23.78	1167.74
30.08	ALI_MAINSTREET	168+01.10	-21.50	1167.71
30.09	ALI_MAINSTREET	168+01.17	-28.78	1167.80
30.10	ALI_MAINSTREET	168+01.18	-29.81	1167.83
30.11	ALI_MAINSTREET	167+96.43	-28.82	1167.59
30.12	ALI_MAINSTREET	167+96.38	-29.83	1167.69
30.13	ALI_MAINSTREET	167+96.15	-34.80	1167.74
30.14	ALI_MAINSTREET	167+91.44	-34.85	1167.69
30.15	ALI_MAINSTREET	167+91.43	-28.87	1167.54
30.16	ALI_MAINSTREET	167+89.43	-28.89	1167.40
30.17	ALI_MAINSTREET	167+86.41	-28.92	1167.35
30.18	ALI_MAINSTREET	167+83.72	-28.97	1167.41
30.19	ALI_MAINSTREET	167+83.56	-31.82	1167.55





LEGEND

	EROSION MAT URBAN CLASS I TYPE A
	INLET PROTECTION (TYPE A AND C)

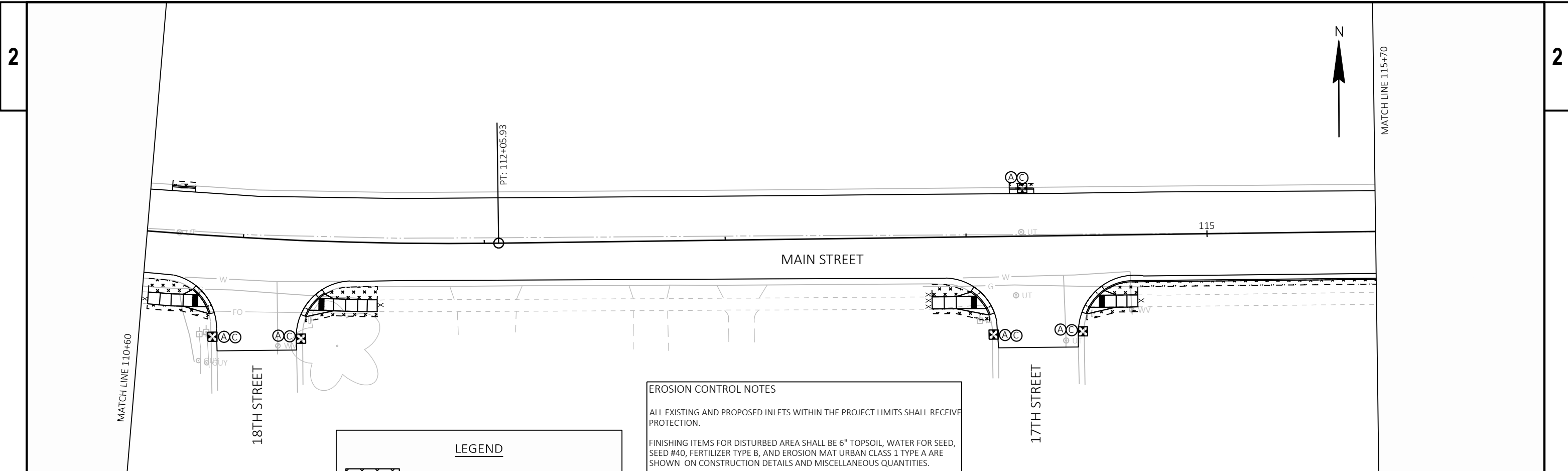
EROSION CONTROL NOTES

ALL EXISTING AND PROPOSED INLETS WITHIN THE PROJECT LIMITS SHALL RECEIVE PROTECTION.

FINISHING ITEMS FOR DISTURBED AREA SHALL BE 6" TOPSOIL, WATER FOR SEED, SEED #40, FERTILIZER TYPE B, AND EROSION MAT URBAN CLASS 1 TYPE A ARE SHOWN ON CONSTRUCTION DETAILS AND MISCELLANEOUS QUANTITIES.

URBAN SECTIONS SHALL RECEIVE PREP FOR LAWN TYPE TURF.

ANY INLETS THAT WILL NOT BE OPEN TO DRAINAGE AFTER EXISTING CURB AND GUTTER IS REMOVED DO NOT REQUIRE A TYPE A INLET PROTECTION.



LEGEND

	EROSION MAT URBAN CLASS I TYPE A
	INLET PROTECTION (TYPE A AND C)

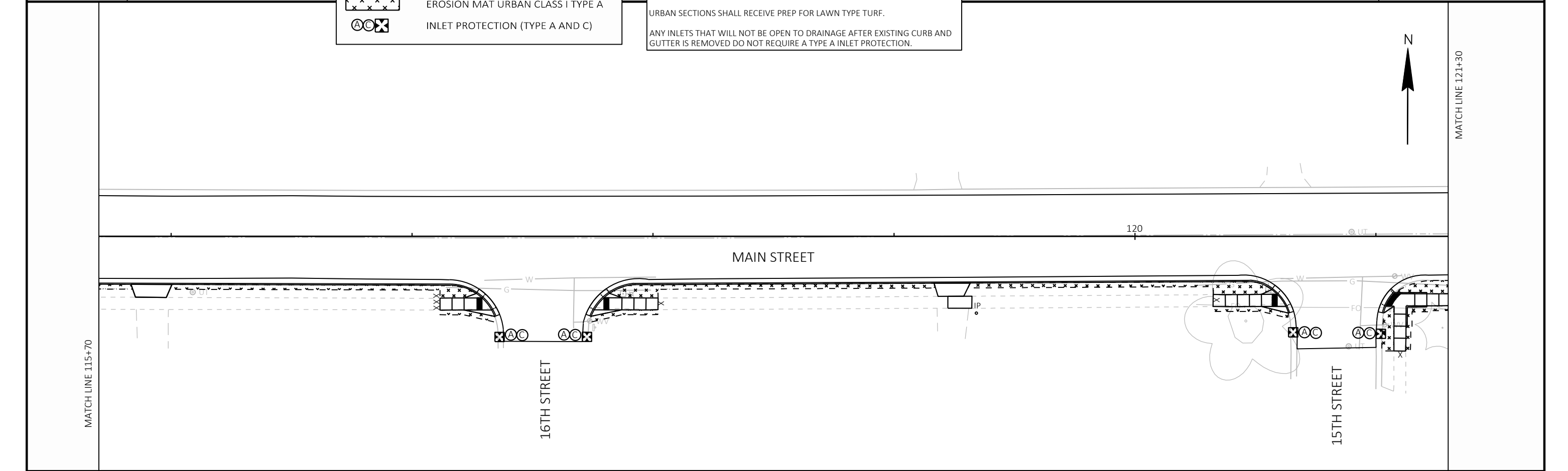
EROSION CONTROL NOTES

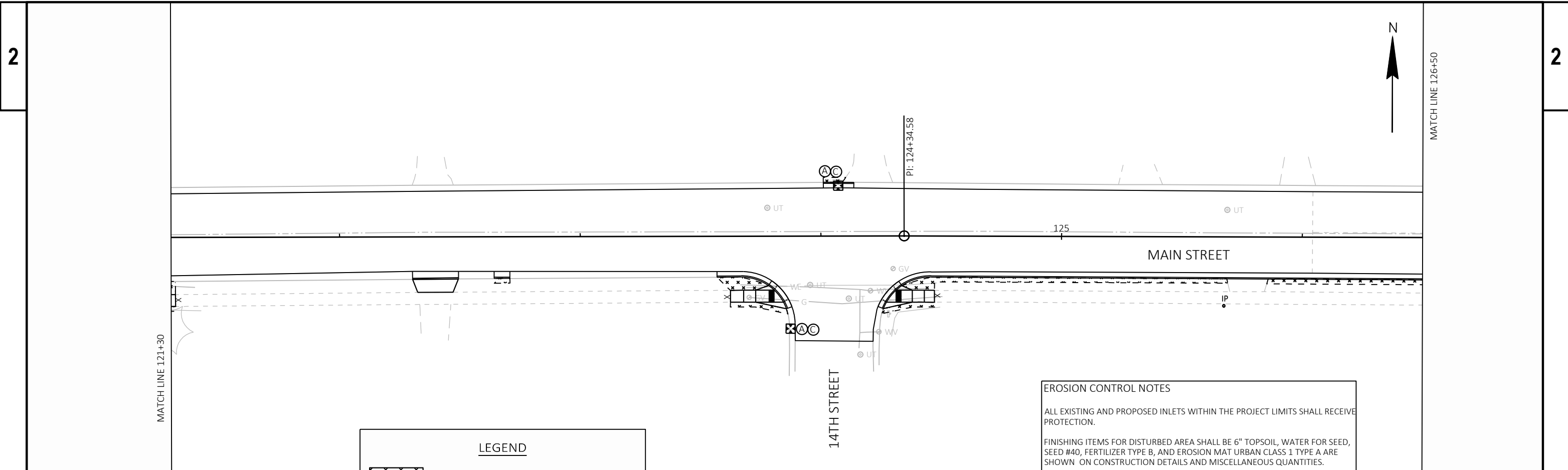
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ANY INLETS THAT WILL NOT BE OPEN TO DRAINAGE AFTER EXISTING CURB AND GUTTER IS REMOVED DO NOT REQUIRE A TYPE A INLET PROTECTION.





LEGEND

	EROSION MAT URBAN CLASS I TYPE A
	INLET PROTECTION (TYPE A AND C)

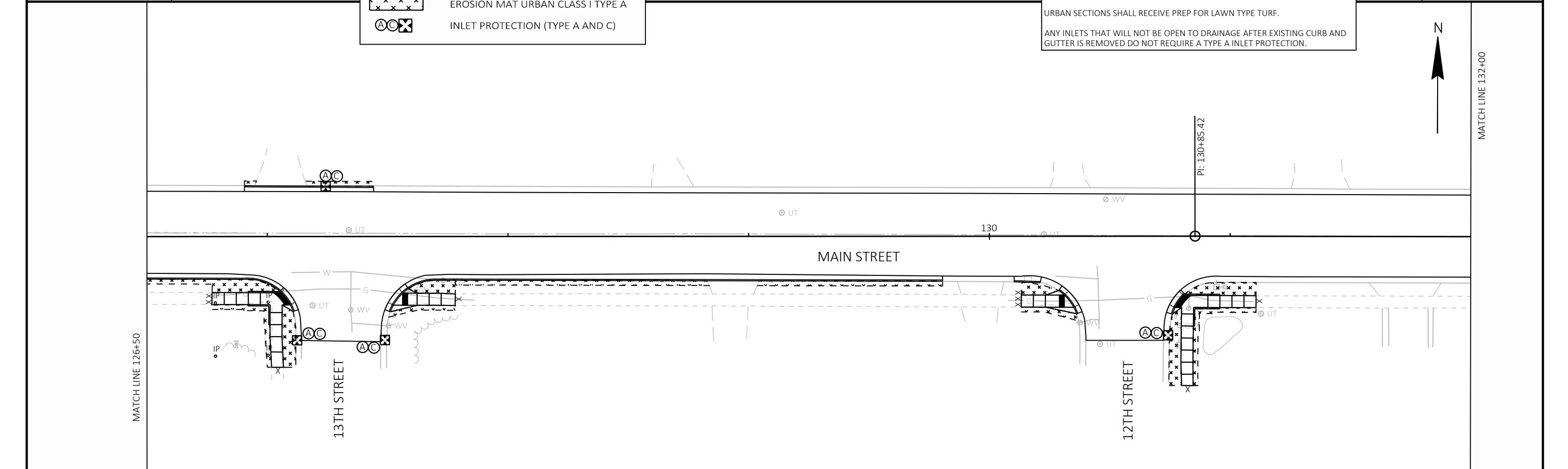
EROSION CONTROL NOTES

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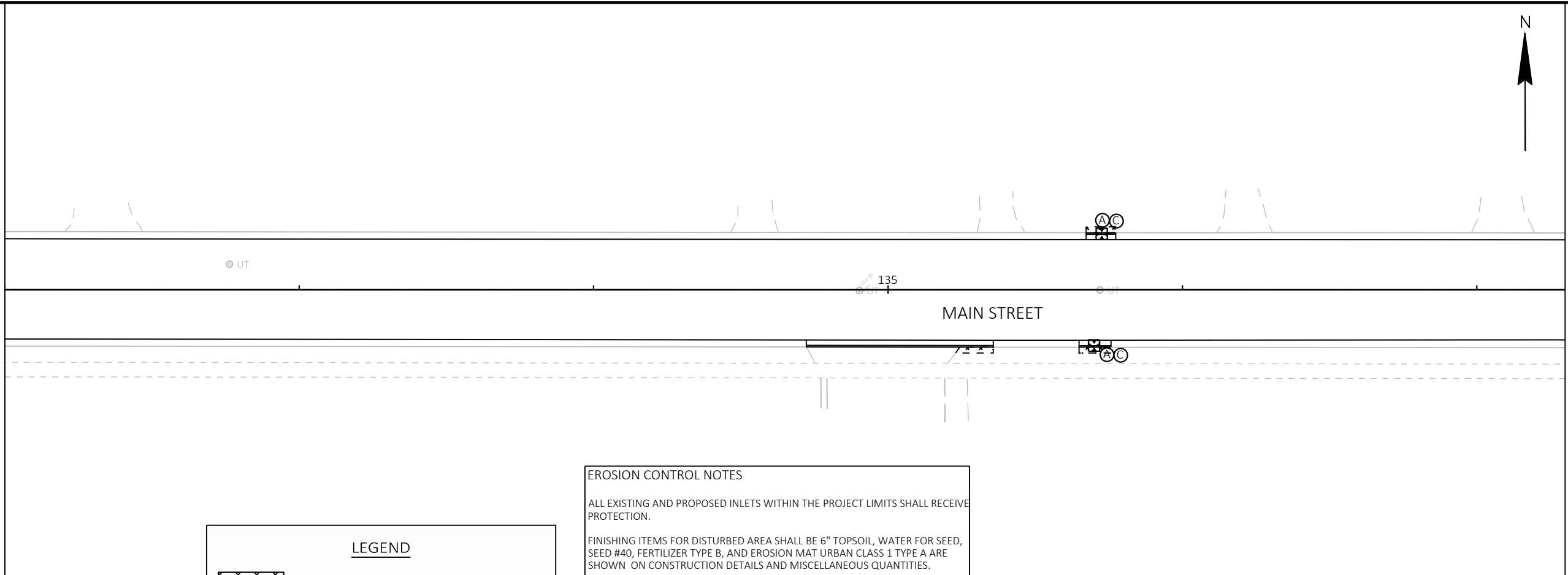
PROJECT NO: 6999-16-73	ROADWAY: MAIN STREET	COUNTY: MARATHON	EROSION CONTROL	SHEET	E
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2

2

MATCH LINE 132+00

MATCH LINE 137+30



LEGEND

 EROSION MAT URBAN CLASS I TYPE A
 INLET PROTECTION (TYPE A AND C)

EROSION CONTROL NOTES

ALL EXISTING AND PROPOSED INLETS WITHIN THE PROJECT LIMITS SHALL RECEIVE PROTECTION.

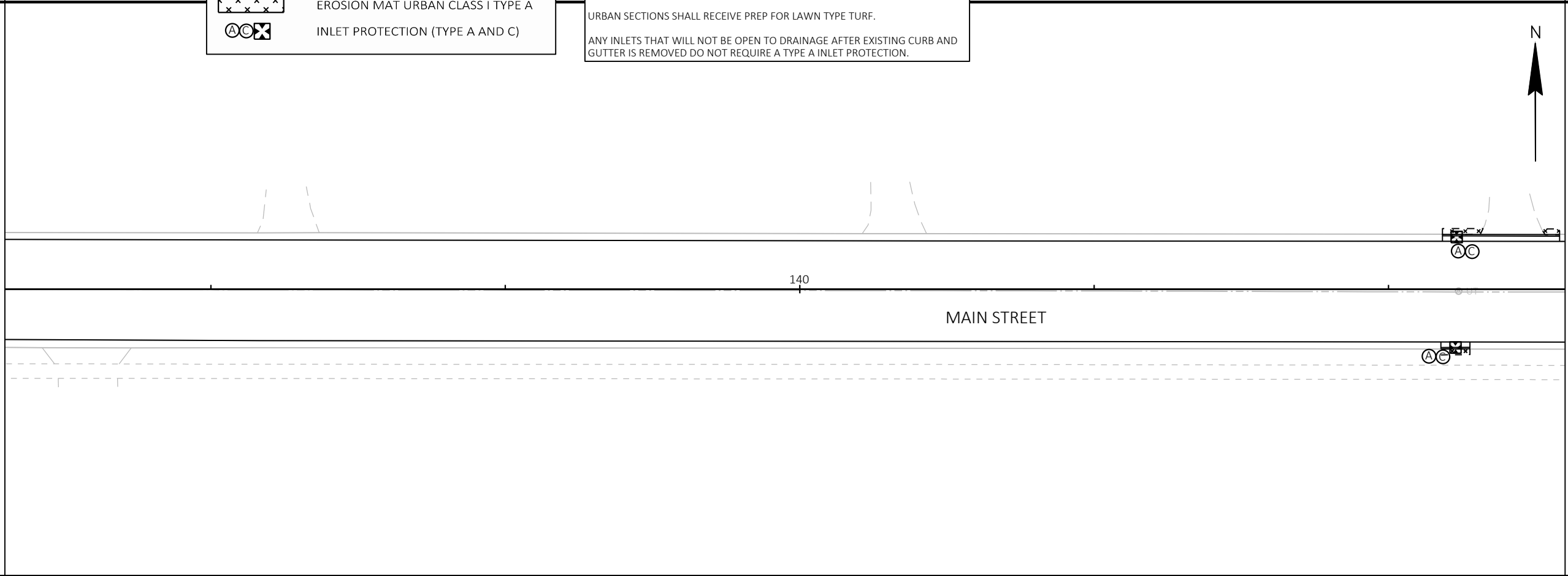
FINISHING ITEMS FOR DISTURBED AREA SHALL BE 6" TOPSOIL, WATER FOR SEED, SEED #40, FERTILIZER TYPE B, AND EROSION MAT URBAN CLASS 1 TYPE A ARE SHOWN ON CONSTRUCTION DETAILS AND MISCELLANEOUS QUANTITIES.

URBAN SECTIONS SHALL RECEIVE PREP FOR LAWN TYPE TURF.

ANY INLETS THAT WILL NOT BE OPEN TO DRAINAGE AFTER EXISTING CURB AND GUTTER IS REMOVED DO NOT REQUIRE A TYPE A INLET PROTECTION.

MATCH LINE 137+30

MATCH LINE 142+60



PROJECT NO: 6999-16-73

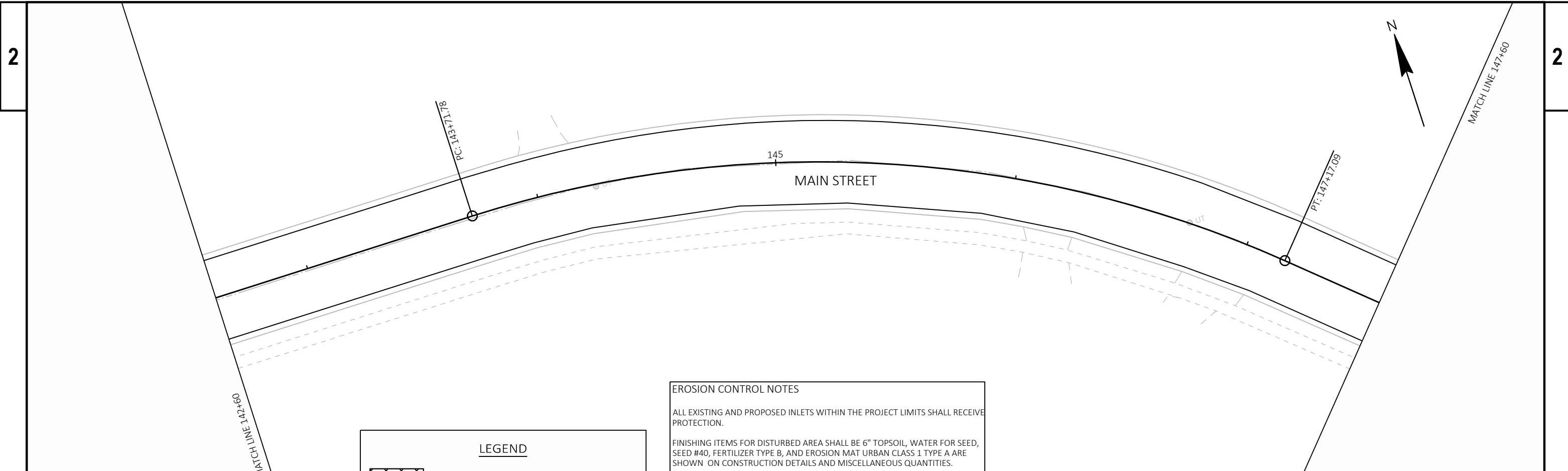
ROADWAY: MAIN STREET

COUNTY: MARATHON

EROSION CONTROL

SHEET

E



LEGEND

	EROSION MAT URBAN CLASS I TYPE A
	INLET PROTECTION (TYPE A AND C)

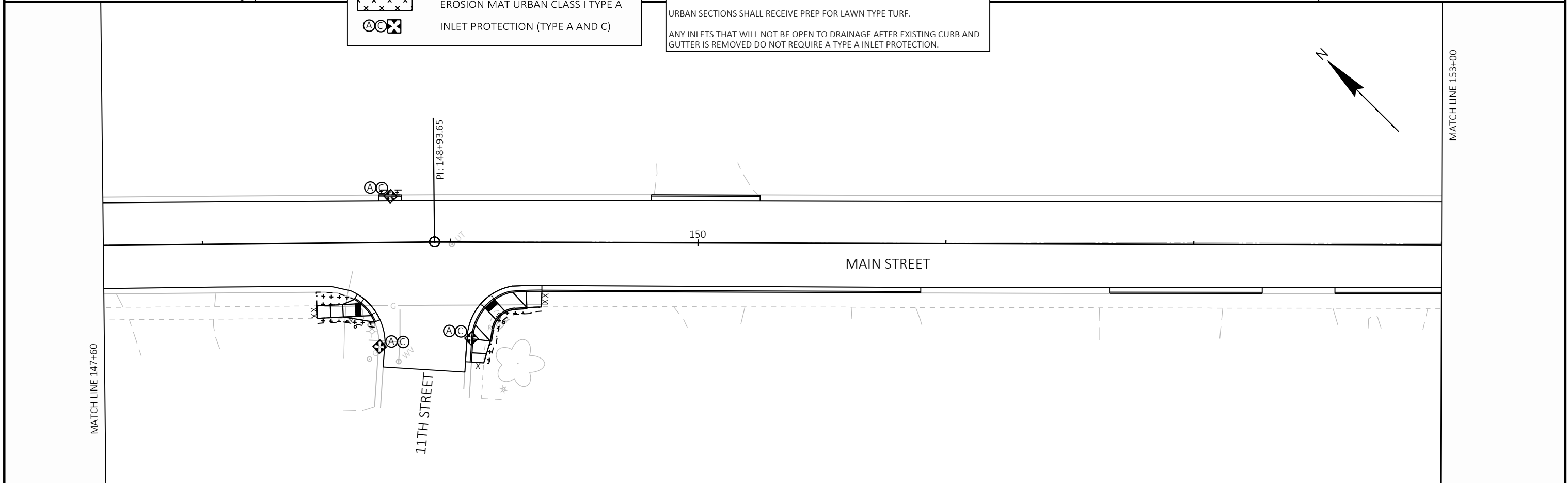
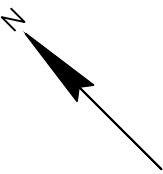
EROSION CONTROL NOTES

ALL EXISTING AND PROPOSED INLETS WITHIN THE PROJECT LIMITS SHALL RECEIVE PROTECTION.

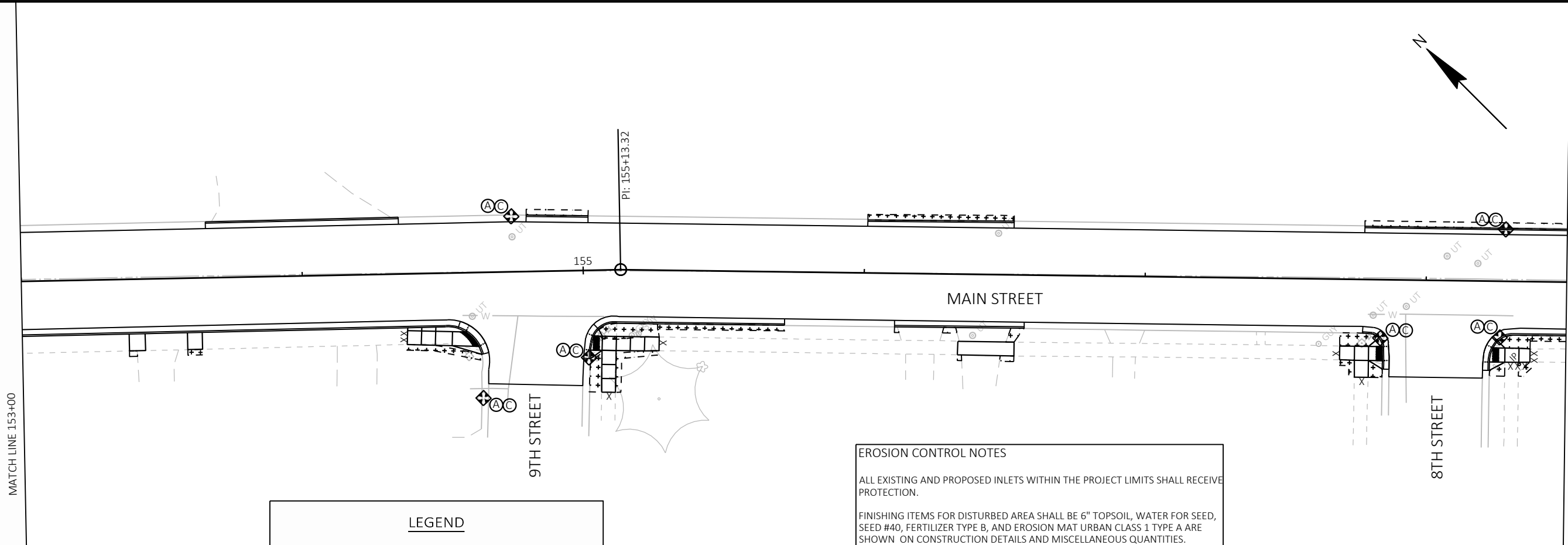
FINISHING ITEMS FOR DISTURBED AREA SHALL BE 6" TOPSOIL, WATER FOR SEED, SEED #40, FERTILIZER TYPE B, AND EROSION MAT URBAN CLASS 1 TYPE A ARE SHOWN ON CONSTRUCTION DETAILS AND MISCELLANEOUS QUANTITIES.

URBAN SECTIONS SHALL RECEIVE PREP FOR LAWN TYPE TURF.

ANY INLETS THAT WILL NOT BE OPEN TO DRAINAGE AFTER EXISTING CURB AND GUTTER IS REMOVED DO NOT REQUIRE A TYPE A INLET PROTECTION.



PROJECT NO: 6999-16-73	ROADWAY: MAIN STREET	COUNTY: MARATHON	EROSION CONTROL	SHEET	E
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MATCH LINE 153+00

MATCH LINE 158+50

LEGEND

	EROSION MAT URBAN CLASS I TYPE A
	INLET PROTECTION (TYPE A AND C)

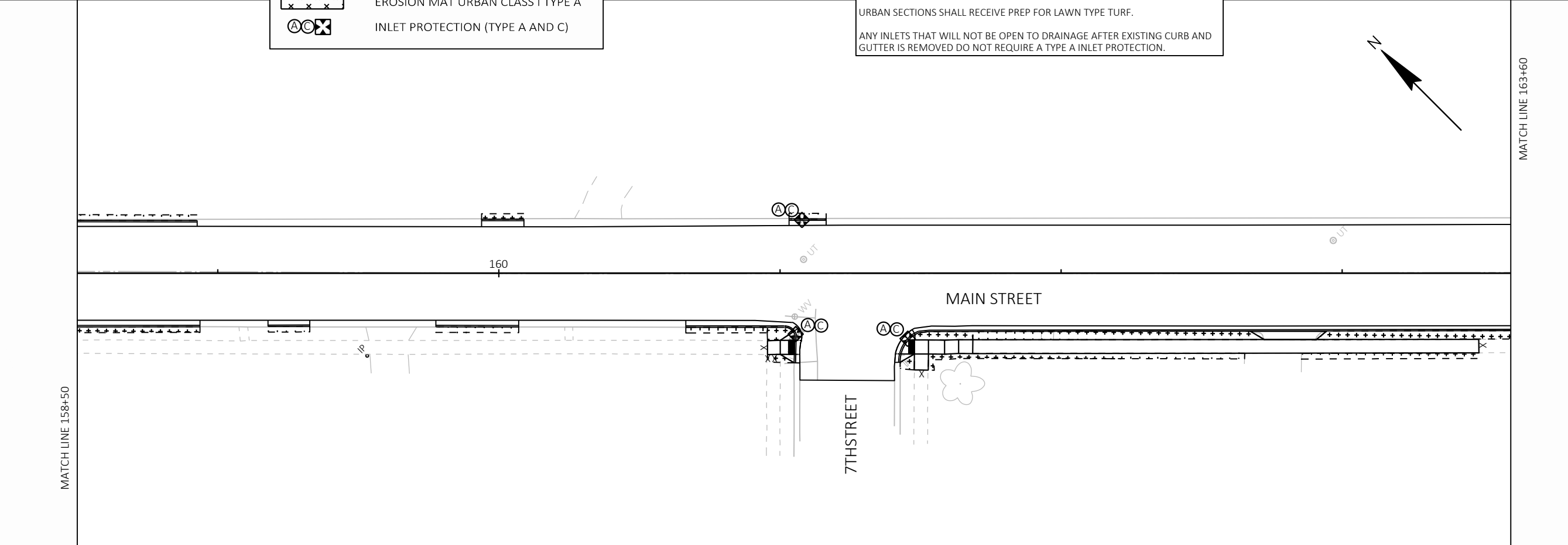
EROSION CONTROL NOTES

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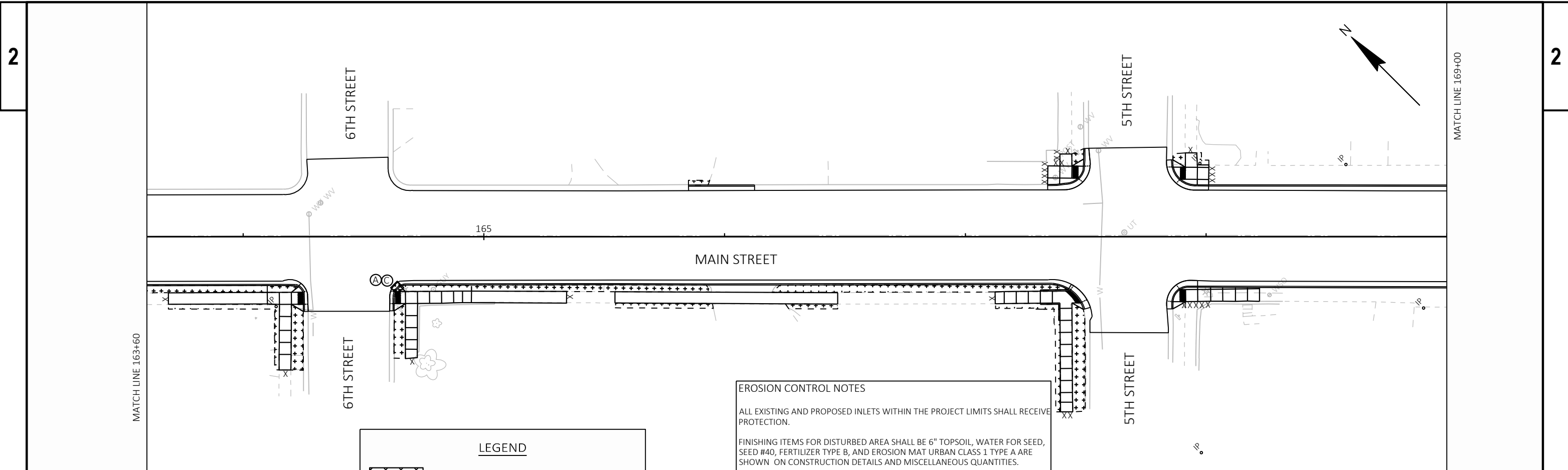
URBAN SECTIONS SHALL RECEIVE PREP FOR LAWN TYPE TURF.

ANY INLETS THAT WILL NOT BE OPEN TO DRAINAGE AFTER EXISTING CURB AND GUTTER IS REMOVED DO NOT REQUIRE A TYPE A INLET PROTECTION.



MATCH LINE 158+50

MATCH LINE 163+60



LEGEND

	EROSION MAT URBAN CLASS I TYPE A
	INLET PROTECTION (TYPE A AND C)

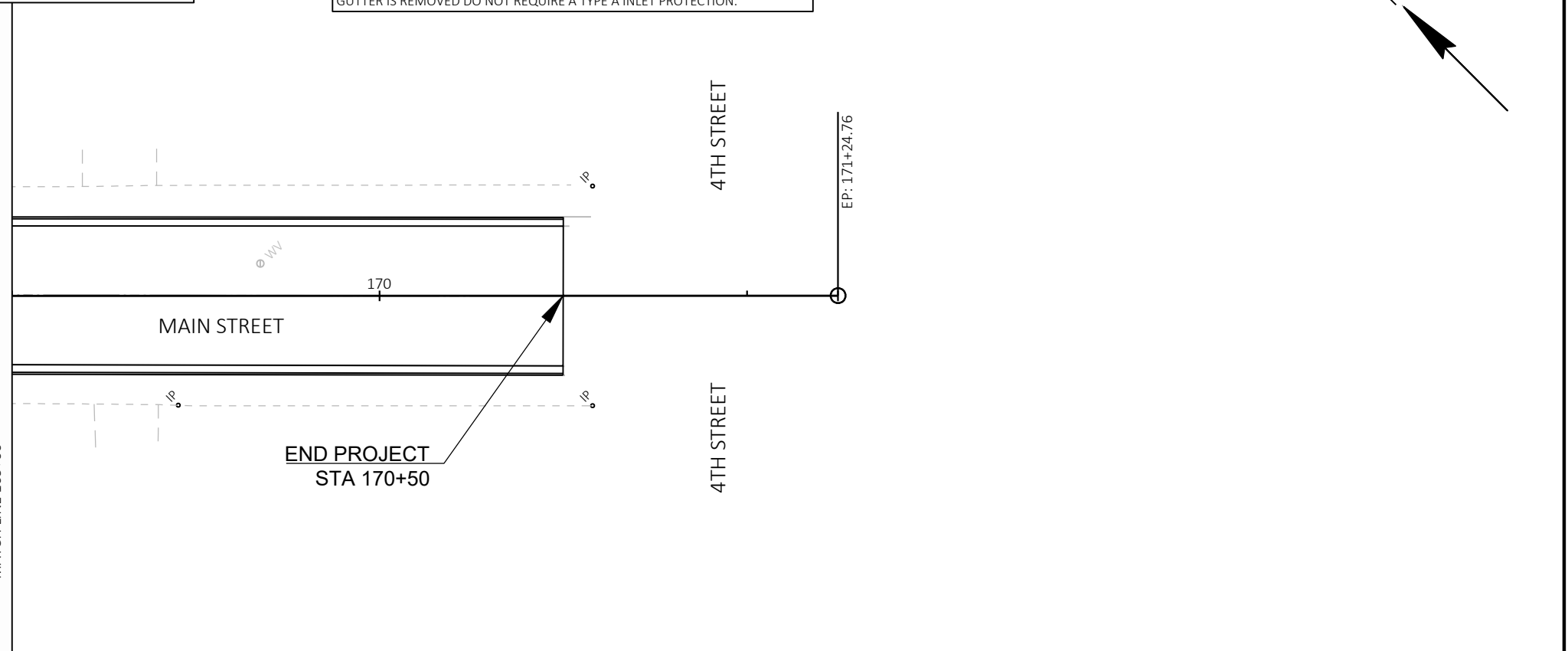
EROSION CONTROL NOTES

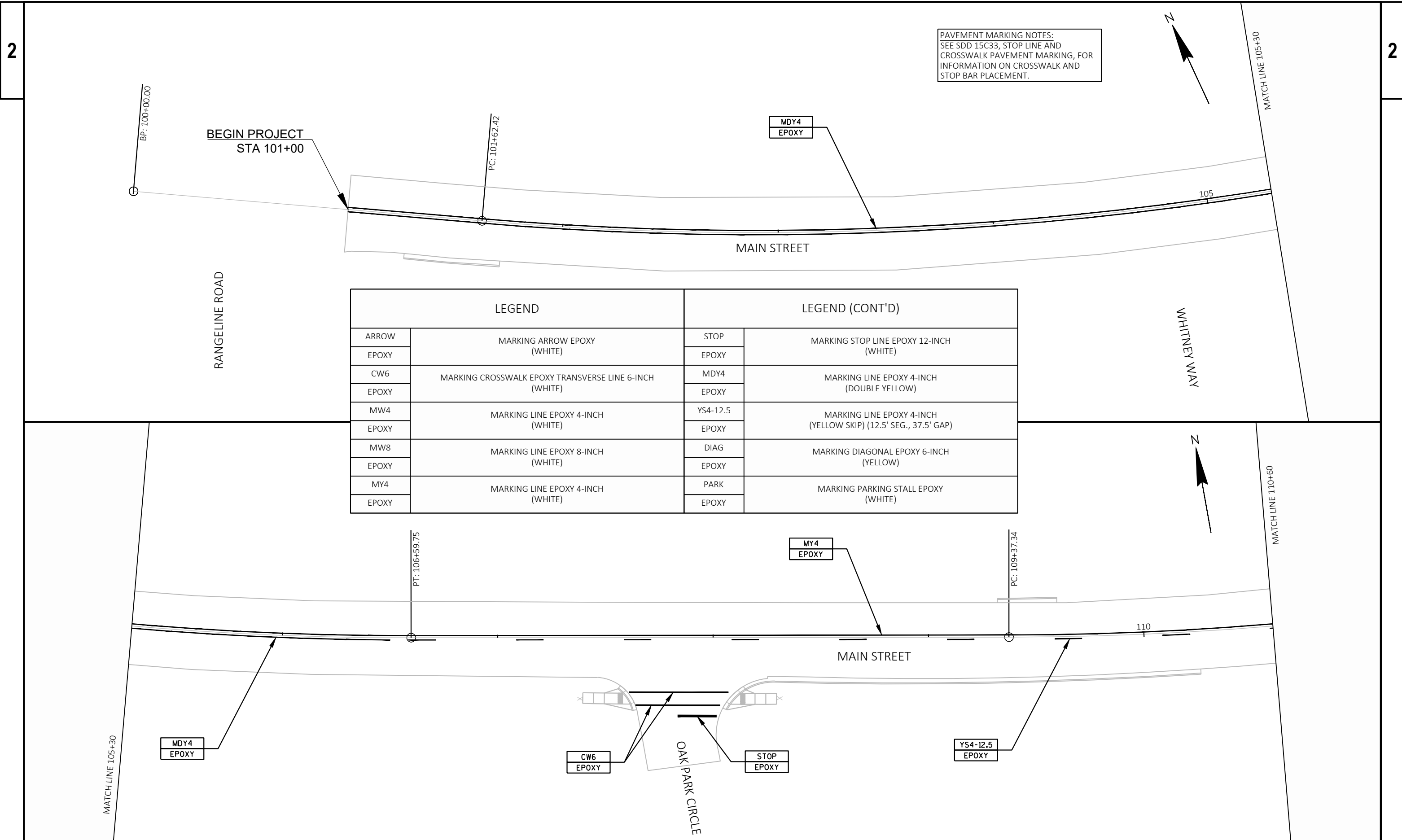
ALL EXISTING AND PROPOSED INLETS WITHIN THE PROJECT LIMITS SHALL RECEIVE PROTECTION.

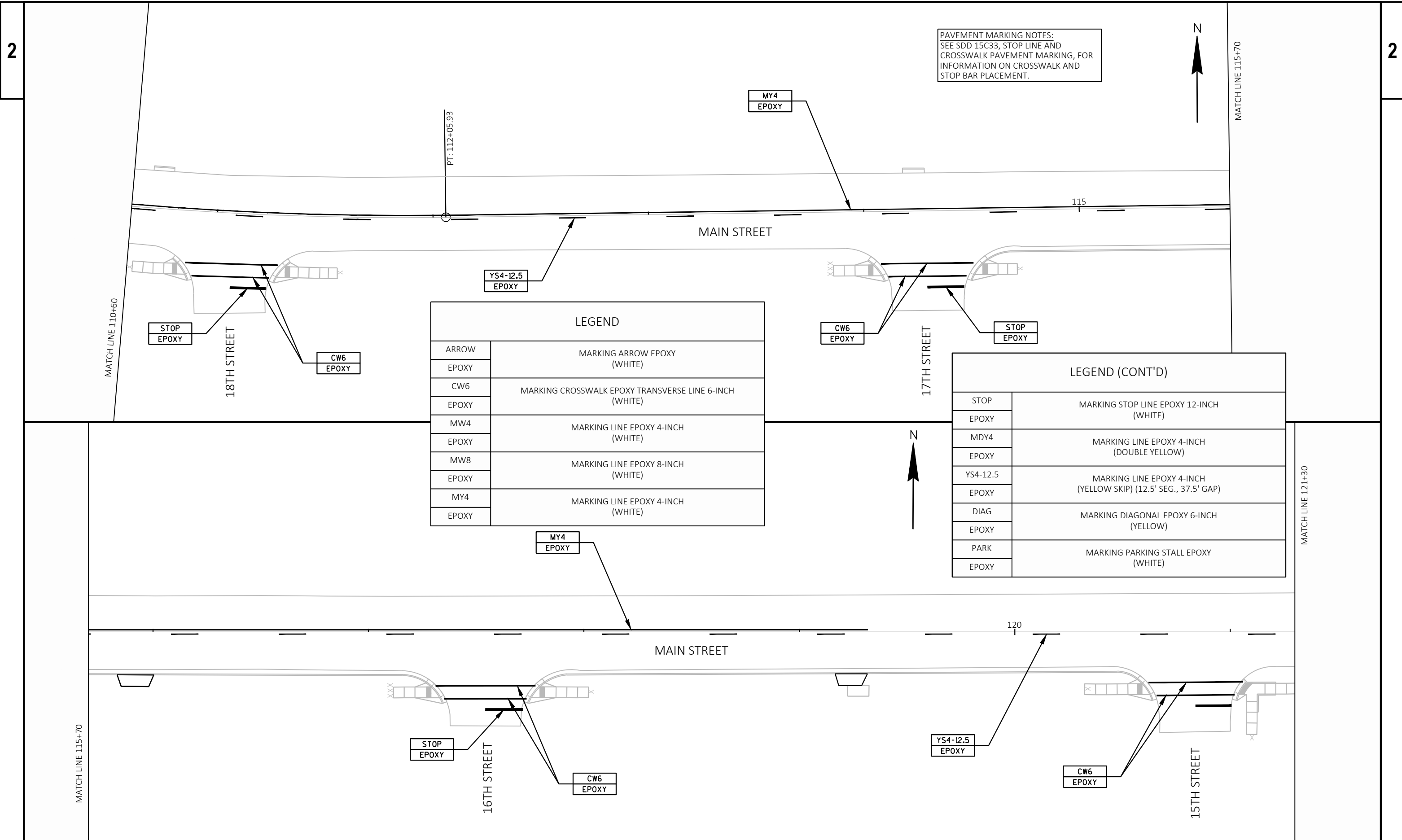
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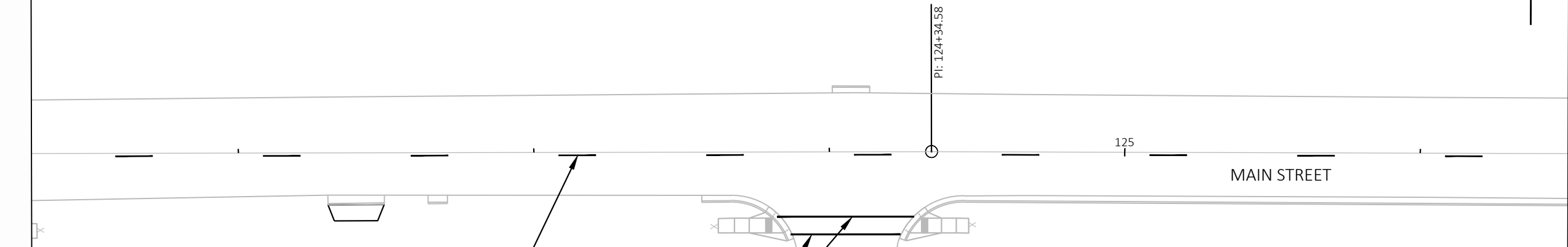


PAVEMENT MARKING NOTES:
SEE SDD 15C33, STOP LINE AND
CROSSWALK PAVEMENT MARKING, FOR
INFORMATION ON CROSSWALK AND
STOP BAR PLACEMENT.

LEGEND	
ARROW	MARKING ARROW EPOXY (WHITE)
EPOXY	
CW6	MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)
EPOXY	
MW4	MARKING LINE EPOXY 4-INCH (WHITE)
EPOXY	
MW8	MARKING LINE EPOXY 8-INCH (WHITE)
EPOXY	
MY4	MARKING LINE EPOXY 4-INCH (WHITE)
EPOXY	

LEGEND (CONT'D)	
STOP	MARKING STOP LINE EPOXY 12-INCH (WHITE)
EPOXY	
MDY4	MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW)
EPOXY	
YS4-12.5	MARKING LINE EPOXY 4-INCH (YELLOW SKIP) (12.5' SEG., 37.5' GAP)
EPOXY	
DIAG	MARKING DIAGONAL EPOXY 6-INCH (YELLOW)
EPOXY	
PARK	MARKING PARKING STALL EPOXY (WHITE)
EPOXY	

PAVEMENT MARKING NOTES:
 SEE SDD 15C33, STOP LINE AND
 CROSSWALK PAVEMENT MARKINGS, FOR
 INFORMATION ON CROSSWALK AND
 STOP BAR PLACEMENT.

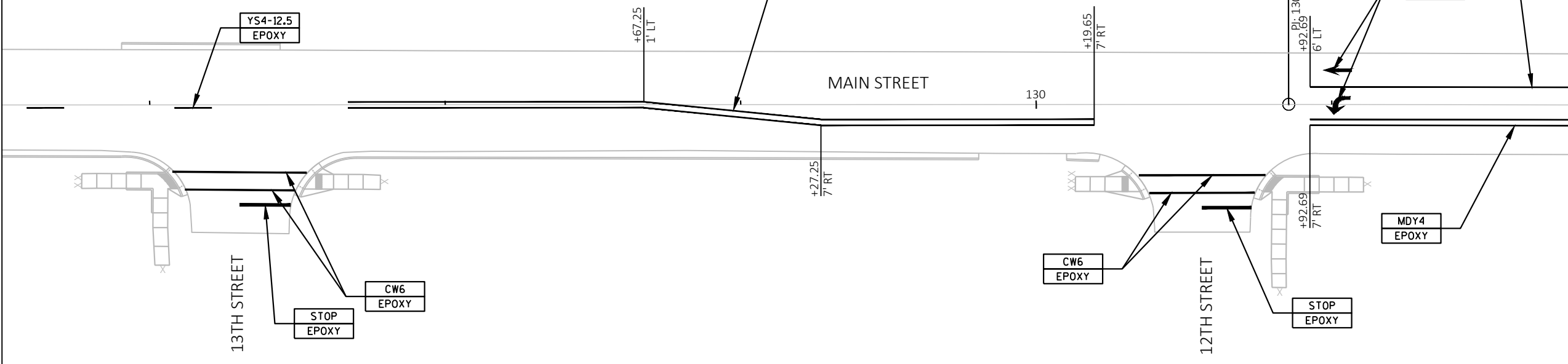


MATCH LINE 121+30

MATCH LINE 126+50

LEGEND	
ARROW EPOXY	MARKING ARROW EPOXY (WHITE)
CW6 EPOXY	MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)
MW4 EPOXY	MARKING LINE EPOXY 4-INCH (WHITE)
MW8 EPOXY	MARKING LINE EPOXY 8-INCH (WHITE)
MY4 EPOXY	MARKING LINE EPOXY 4-INCH (WHITE)

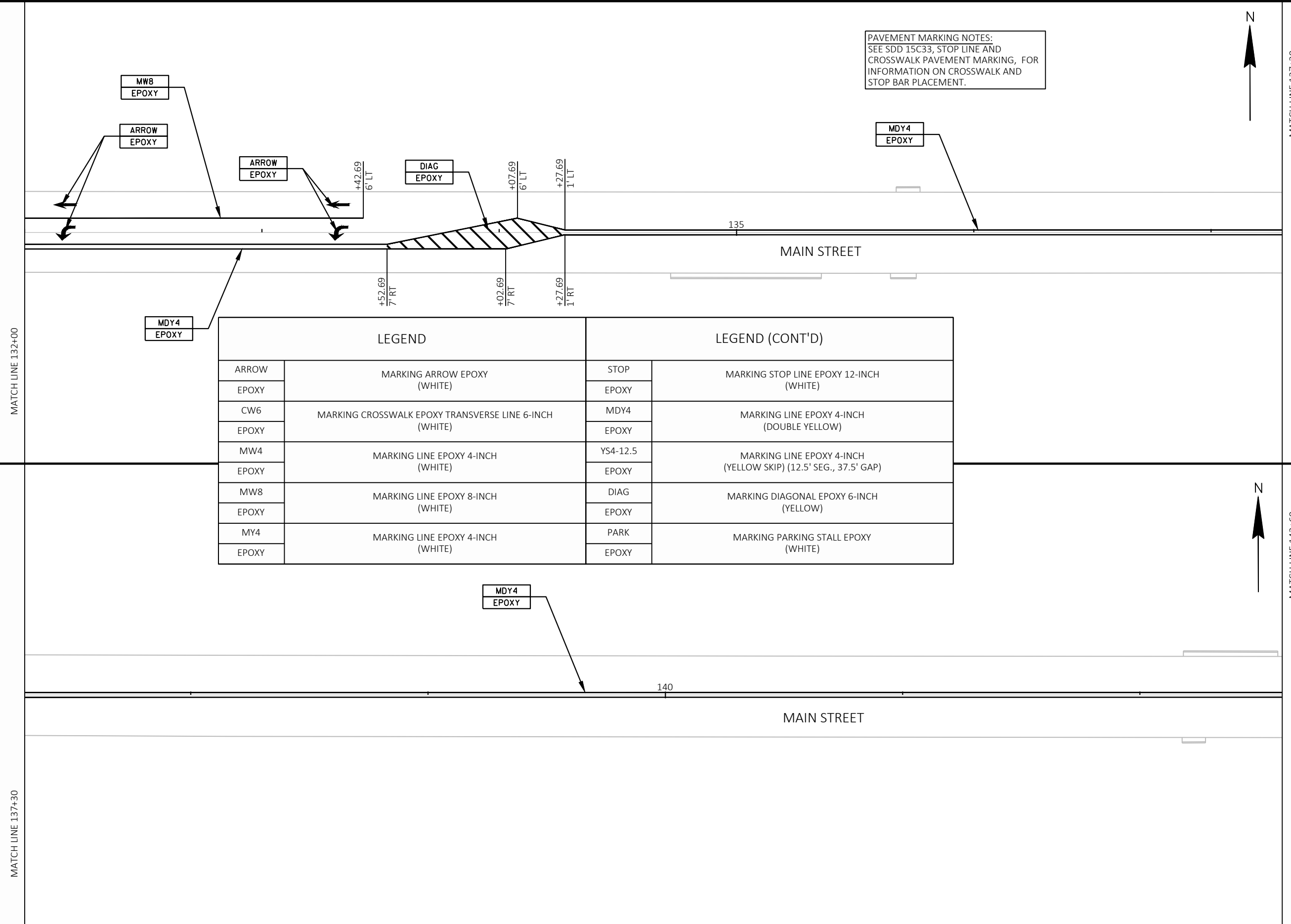
LEGEND (CONT'D)	
STOP EPOXY	MARKING STOP LINE EPOXY 12-INCH (WHITE)
MDY4 EPOXY	MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW)
YS4-12.5 EPOXY	MARKING LINE EPOXY 4-INCH (YELLOW SKIP) (12.5' SEG., 37.5' GAP)
DIAG EPOXY	MARKING DIAGONAL EPOXY 6-INCH (YELLOW)
PARK EPOXY	MARKING PARKING STALL EPOXY (WHITE)



MATCH LINE 126+50

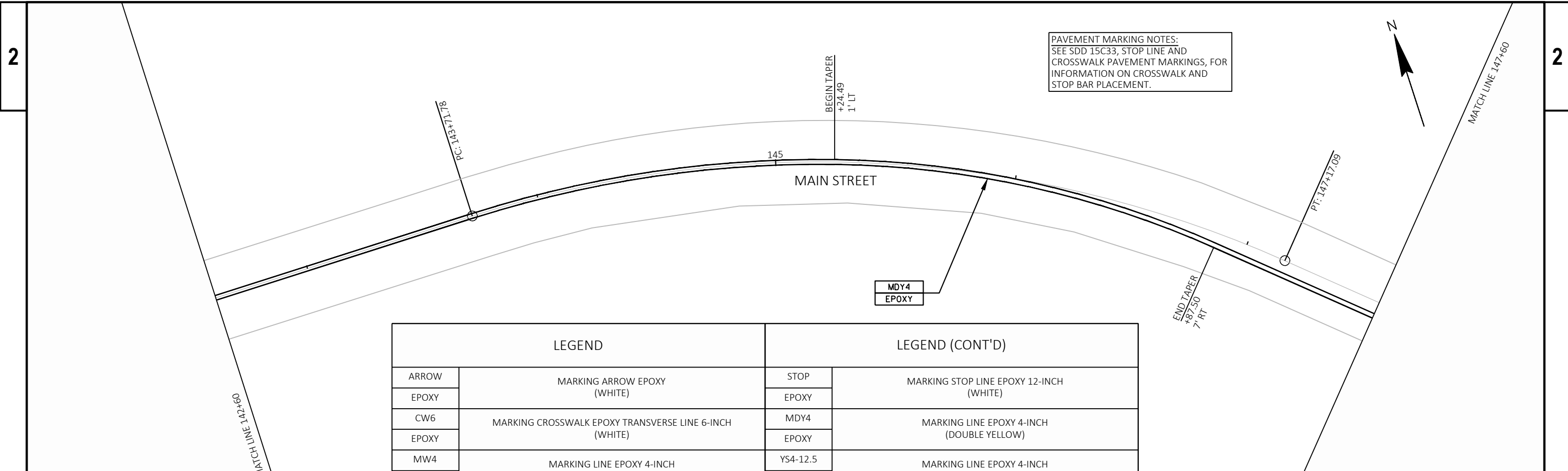
MATCH LINE 132+00

PAVEMENT MARKING NOTES:
 SEE SDD 15C33, STOP LINE AND
 CROSSWALK PAVEMENT MARKING, FOR
 INFORMATION ON CROSSWALK AND
 STOP BAR PLACEMENT.

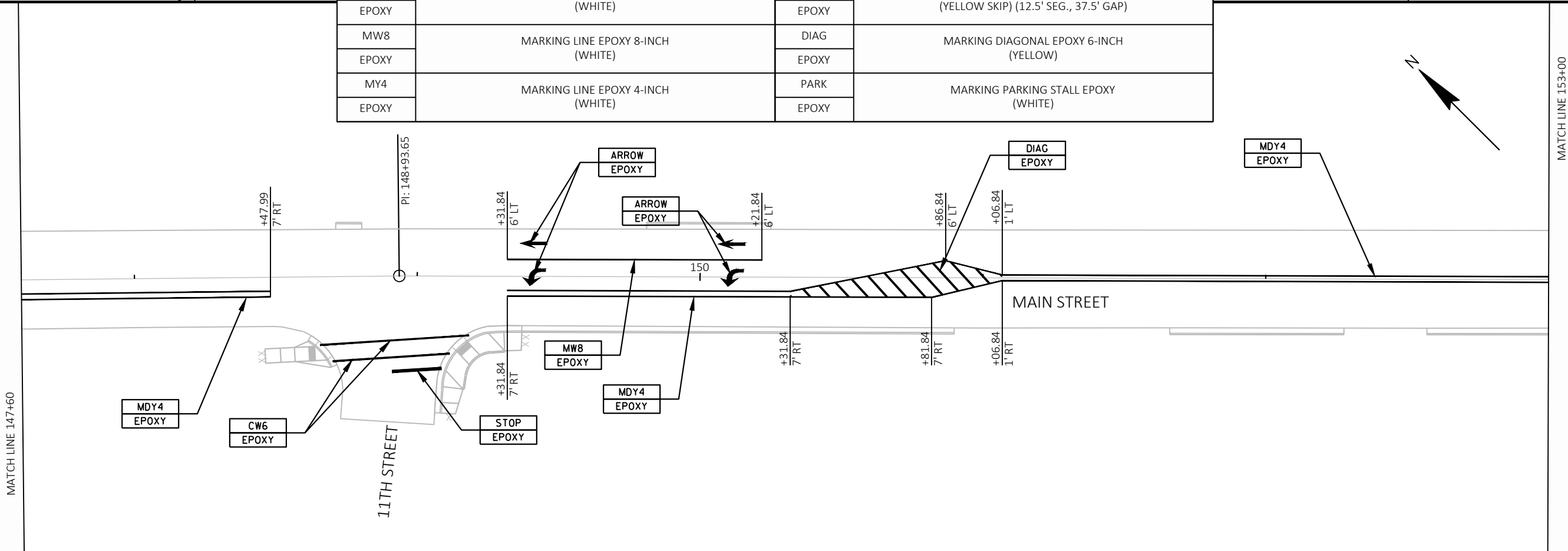


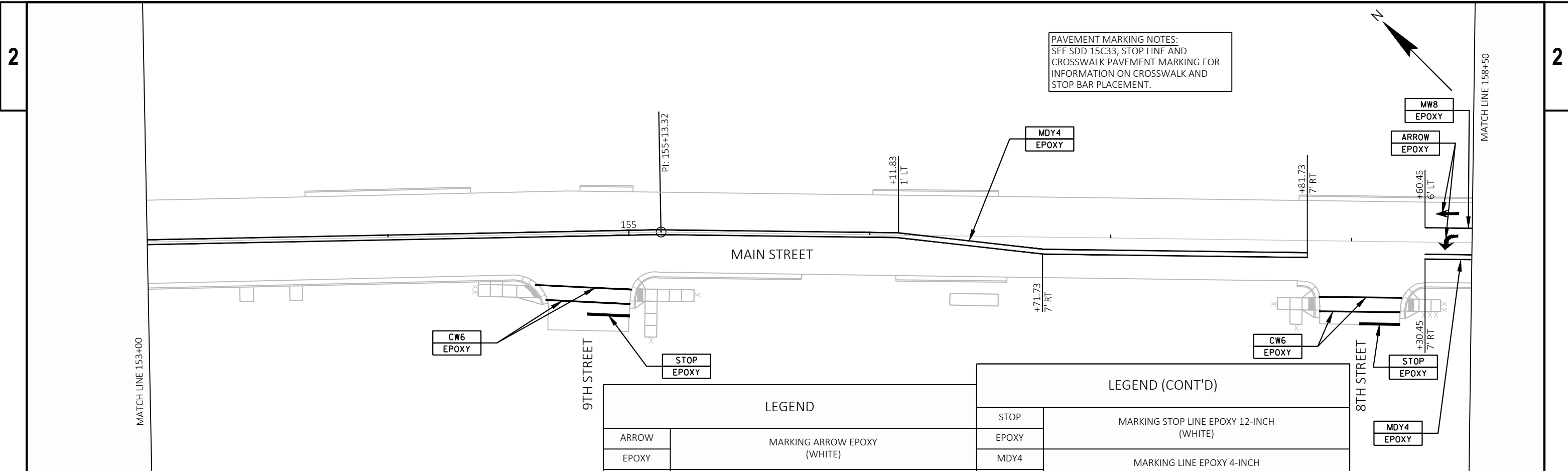
LEGEND		LEGEND (CONT'D)	
ARROW EPOXY	MARKING ARROW EPOXY (WHITE)	STOP EPOXY	MARKING STOP LINE EPOXY 12-INCH (WHITE)
CW6 EPOXY	MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)	MDY4 EPOXY	MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW)
MW4 EPOXY	MARKING LINE EPOXY 4-INCH (WHITE)	YS4-12.5 EPOXY	MARKING LINE EPOXY 4-INCH (YELLOW SKIP) (12.5' SEG., 37.5' GAP)
MW8 EPOXY	MARKING LINE EPOXY 8-INCH (WHITE)	DIAG EPOXY	MARKING DIAGONAL EPOXY 6-INCH (YELLOW)
MY4 EPOXY	MARKING LINE EPOXY 4-INCH (WHITE)	PARK EPOXY	MARKING PARKING STALL EPOXY (WHITE)





LEGEND		LEGEND (CONT'D)	
ARROW EPOXY	MARKING ARROW EPOXY (WHITE)	STOP EPOXY	MARKING STOP LINE EPOXY 12-INCH (WHITE)
CW6 EPOXY	MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)	MDY4 EPOXY	MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW)
MW4 EPOXY	MARKING LINE EPOXY 4-INCH (WHITE)	YS4-12.5 EPOXY	MARKING LINE EPOXY 4-INCH (YELLOW SKIP) (12.5' SEG., 37.5' GAP)
MW8 EPOXY	MARKING LINE EPOXY 8-INCH (WHITE)	DIAG EPOXY	MARKING DIAGONAL EPOXY 6-INCH (YELLOW)
MY4 EPOXY	MARKING LINE EPOXY 4-INCH (WHITE)	PARK EPOXY	MARKING PARKING STALL EPOXY (WHITE)

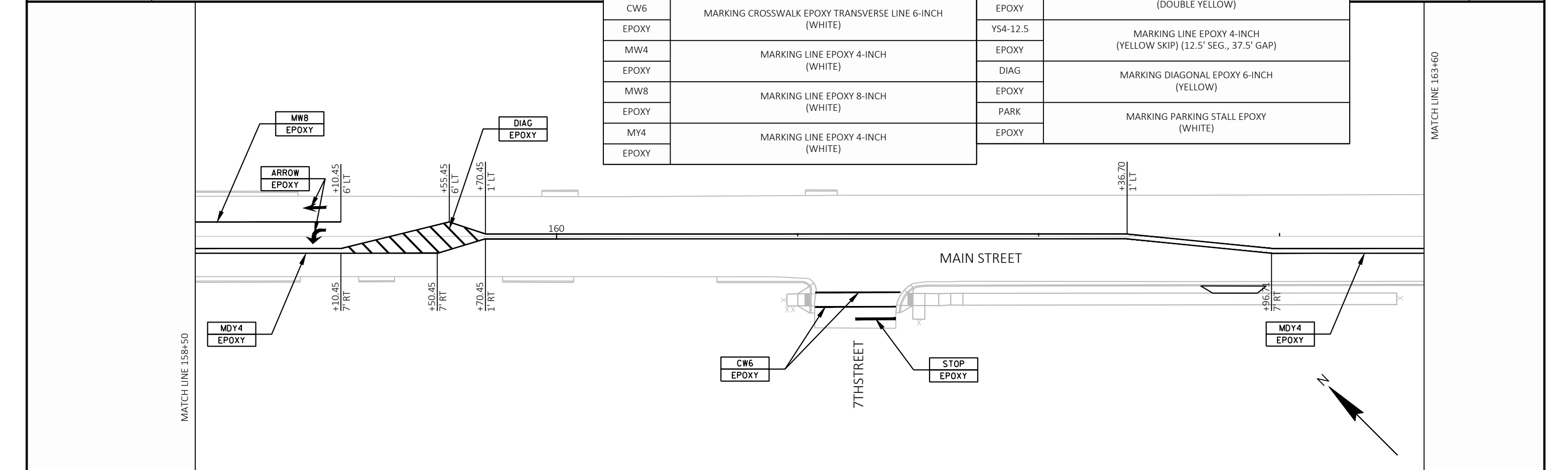




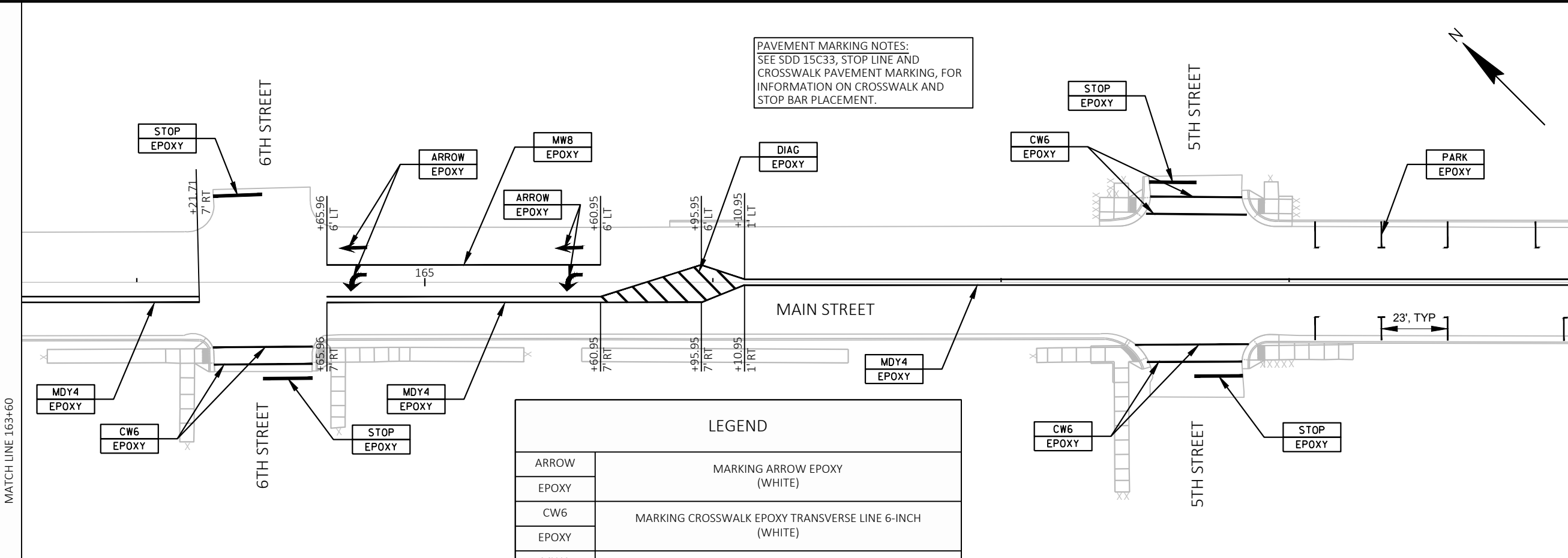
PAVEMENT MARKING NOTES:
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CROSSWALK PAVEMENT MARKING FOR
INFORMATION ON CROSSWALK AND
STOP BAR PLACEMENT.

LEGEND	
ARROW EPOXY	MARKING ARROW EPOXY (WHITE)
CW6 EPOXY	MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)
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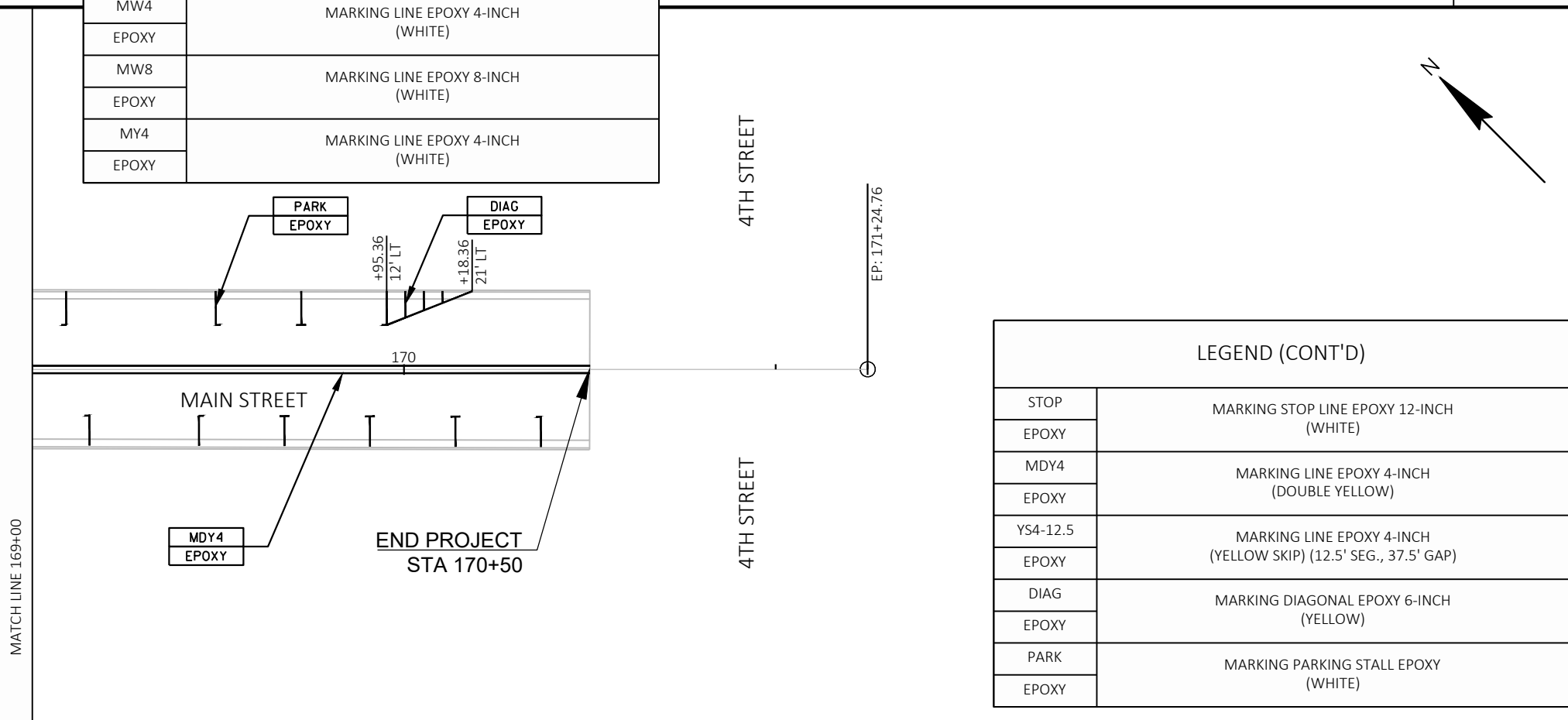
LEGEND (CONT'D)	
STOP EPOXY	MARKING STOP LINE EPOXY 12-INCH (WHITE)
MDY4 EPOXY	MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW)
YS4-12.5 EPOXY	MARKING LINE EPOXY 4-INCH (YELLOW SKIP) (12.5' SEG., 37.5' GAP)
DIAG EPOXY	MARKING DIAGONAL EPOXY 6-INCH (YELLOW)
PARK EPOXY	MARKING PARKING STALL EPOXY (WHITE)



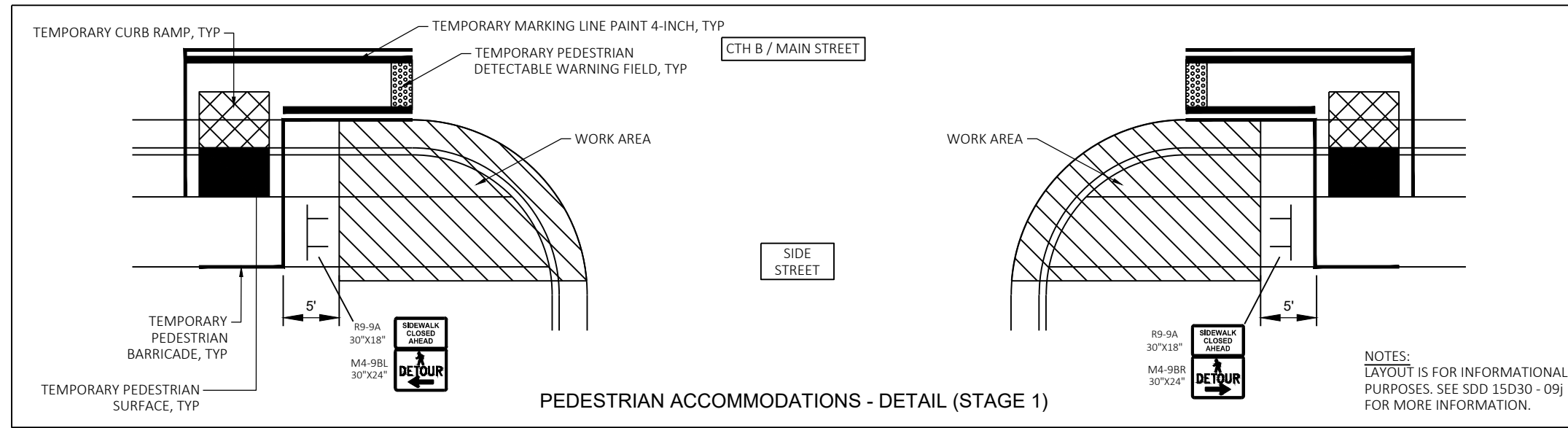
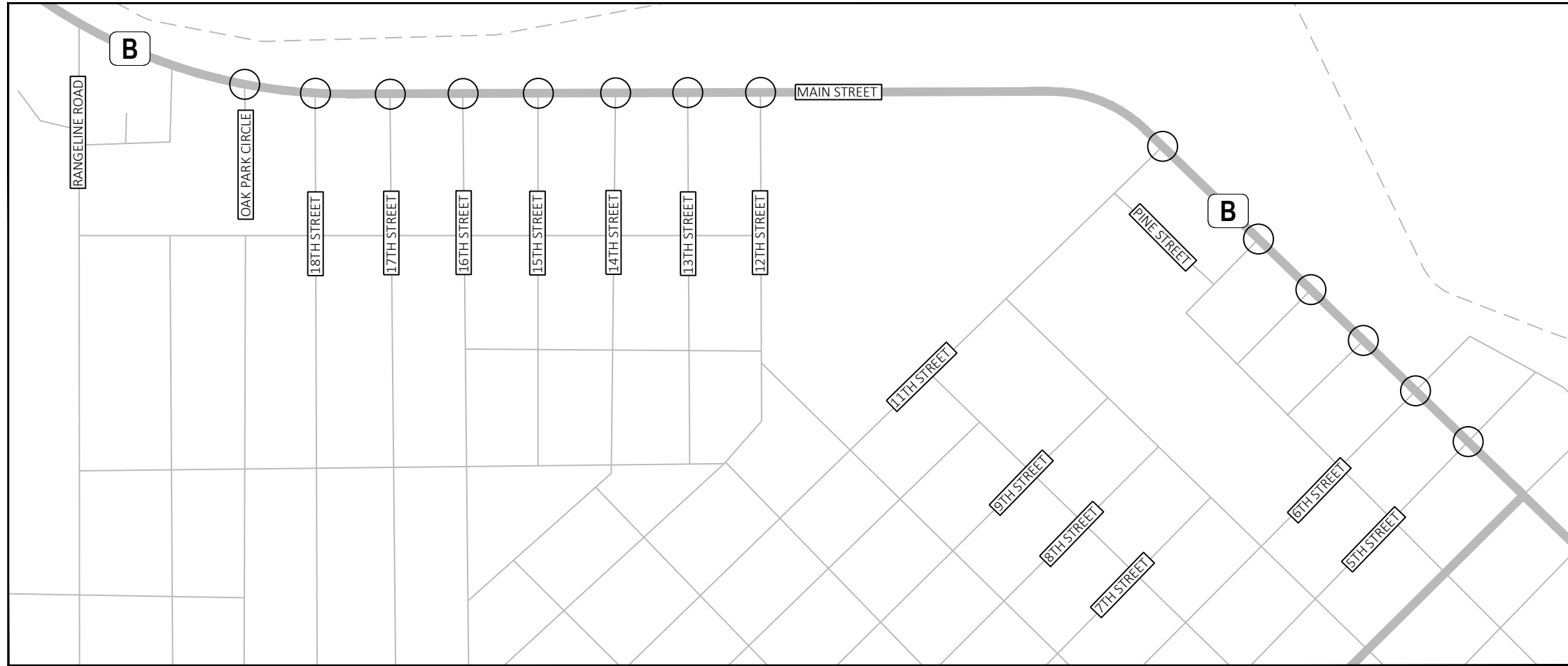
PAVEMENT MARKING NOTES:
 SEE SDD 15C33, STOP LINE AND
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LEGEND	
ARROW EPOXY	MARKING ARROW EPOXY (WHITE)
CW6 EPOXY	MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH (WHITE)
MW4 EPOXY	MARKING LINE EPOXY 4-INCH (WHITE)
MW8 EPOXY	MARKING LINE EPOXY 8-INCH (WHITE)
MY4 EPOXY	MARKING LINE EPOXY 4-INCH (WHITE)



LEGEND (CONT'D)	
STOP EPOXY	MARKING STOP LINE EPOXY 12-INCH (WHITE)
MDY4 EPOXY	MARKING LINE EPOXY 4-INCH (DOUBLE YELLOW)
YS4-12.5 EPOXY	MARKING LINE EPOXY 4-INCH (YELLOW SKIP) (12.5' SEG., 37.5' GAP)
DIAG EPOXY	MARKING DIAGONAL EPOXY 6-INCH (YELLOW)
PARK EPOXY	MARKING PARKING STALL EPOXY (WHITE)



LEGEND:

○	CONSTRUCTION ZONE (SEE DETAIL)
TT	TEMPORARY SUPPORT WITH SIGN

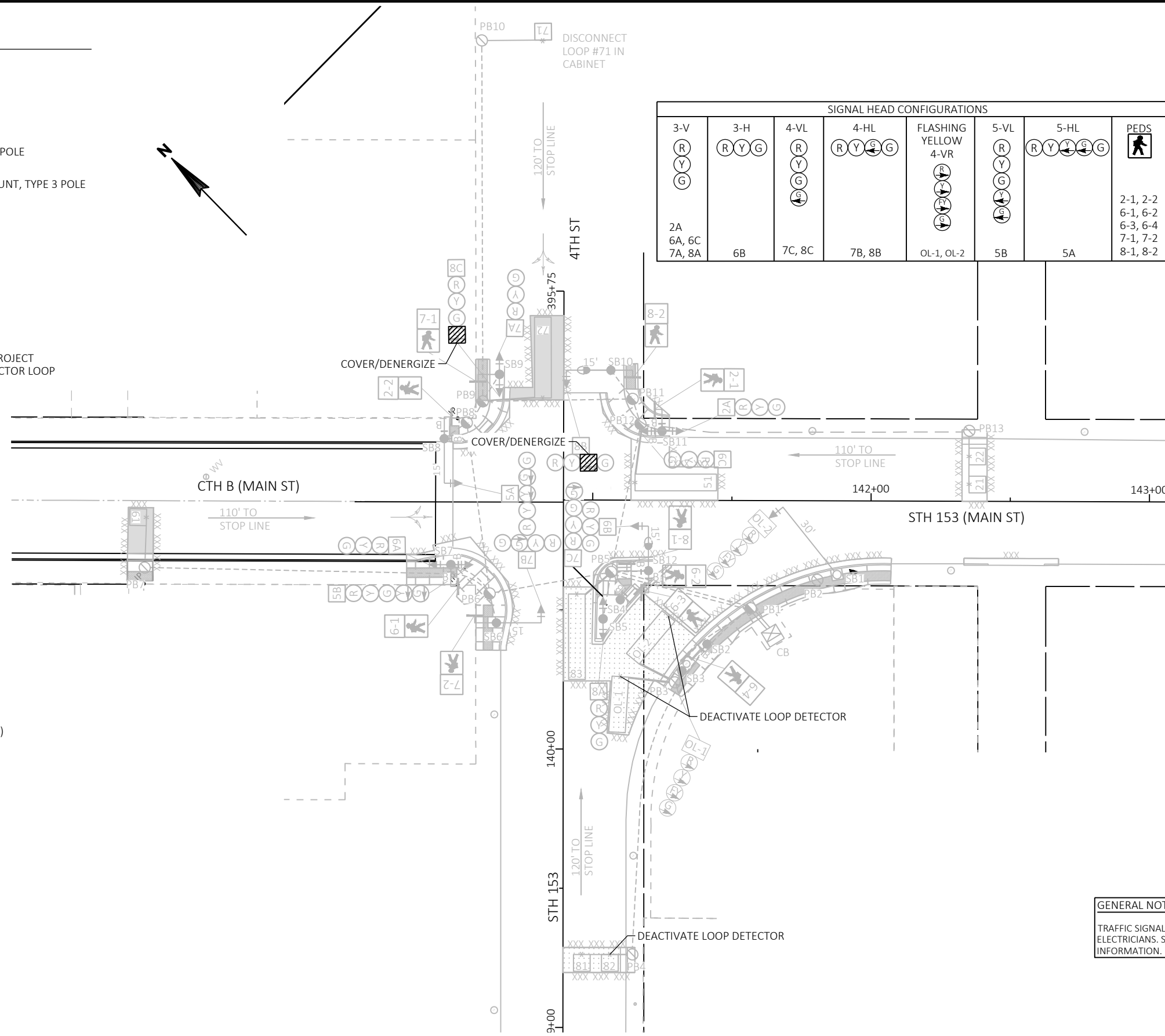
NOTES:
LAYOUT IS FOR INFORMATIONAL PURPOSES. SEE SDD 15D30 - 09j FOR MORE INFORMATION.

LEGEND

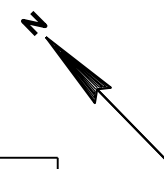
- CONTROL CABINET (TYPE 9)
 - NONMETALLIC CONDUIT
 - SIGNAL HEAD, PEDESTAL MOUNT
TYPE 1 BASE (WITH BACK PLATE)
 - SIGNAL HEAD, MAST ARM MOUNT, TYPE 2 POLE
TYPE 2 BASE (WITH BACKPLATE)
 - SIGNAL HEAD/LUMINAIRE, MAST ARM MOUNT, TYPE 3 POLE
TYPE 2 BASE (WITH BACKPLATE)
 - SIGNAL HEAD TYPE 9 MONOTUBE
TYPE 10 BASE (WITH BACKPLATE)
 - PEDESTRIAN HEAD
 - PUSH BUTTON
 - WISDOT LUMINAIRE
(POWERED OUT OF SIGNAL CABINET)
 - DECORATIVE POST MOUNT LUMINAIRE
(MAINTAINED BY THE CITY)
 - NEW LOOP DETECTORS INSTALLED WITH PROJECT
* LOCATION IS TO FRONT CENTER OF DETECTOR LOOP
 - EXISTING LOOP DETECTOR
(TO REMAIN IN PLACE)
 - PULL BOX, STEEL, 24" X 36"
 - PULL BOX, STEEL, 18" X 24"
 - 2A SIGNAL HEAD NUMBER
 - 4-1 PEDESTRIAN HEAD NUMBER
 - (R) RED CIRCULAR INDICATOR
 - (Y) YELLOW CIRCULAR INDICATOR
 - (G) GREEN CIRCULAR INDICATOR
 - (R) RED ARROW (RIGHT)
 - (Y) YELLOW ARROW (LEFT/RIGHT)
 - (G) GREEN ARROW (LEFT/RIGHT)
 - (FY) FLASHING YELLOW ARROW (RIGHT)
 - LANE DESIGNATION (FOR REFERENCE ONLY)
- NOTE: ALL LENSES ARE 12-INCH LED
GRAYSHADE REPRESENTS EXISTING

MONOTUBE STRUCTURE NUMBERS	
BASE #	STRUCTURE #
SB1	S-37-163

SIGNAL HEAD CONFIGURATIONS							
3-V (R) (Y) (G)	3-H (R) (Y) (G)	4-VL (R) (Y) (G)	4-HL (R) (Y) (G)	FLASHING YELLOW 4-VR (FY)	5-VL (R) (Y) (G)	5-HL (R) (Y) (G)	PEDS (P)
2A 6A, 6C 7A, 8A	6B	7C, 8C	7B, 8B	OL-1, OL-2	5B	5A	2-1, 2-2 6-1, 6-2 6-3, 6-4 7-1, 7-2 8-1, 8-2

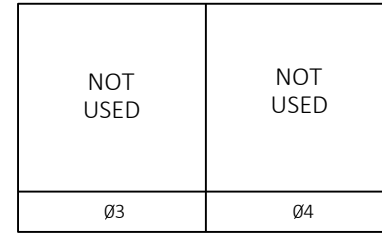
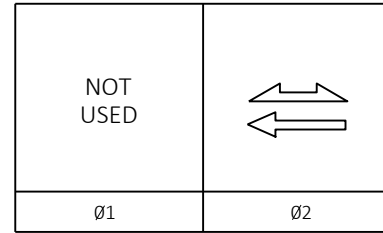


GENERAL NOTES:
TRAFFIC SIGNAL WORK TO BE COMPLETED BY STATE ELECTRICIANS. SEE SPECIAL PROVISIONS FOR MORE INFORMATION.

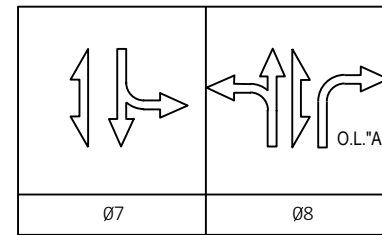
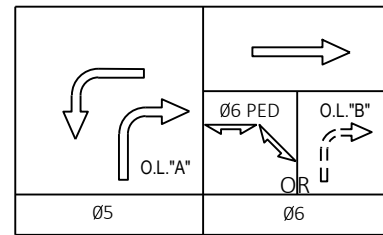


	HEAD NUMBERS	FLASH
Ø1		
Ø2	2A,5A,5B	R
Ø3		
Ø4		
Ø5	5A, 5B	R
Ø6	6A, 6B, 6C	R
Ø7	7A, 7B, 7C	R
Ø8	8A, 8B, 8C	R
O.L."A"	OL-1, OL-2	R
O.L."B"	OL-1, OL-2	R
Ø2 PED	2-1, 2-2	
Ø6 PED	6-1, 6-2,6-3,6-4	
Ø7 PED	7-1, 7-2	
Ø8 PED	8-1, 8-2	

RING 1



RING 2



OLB (NB RIGHT "FLASHING YELLOW ARROW") SHALL BE DISPLAYED WITH PHASE 6 UNLESS "PHASE 6 RED" IS ACTIVATED. IF "PHASE 6 RED" IS ACTIVE, THEN HEADS (OL-1, OL-2) SHALL DISPLAY A "STEADY RIGHT-TURN RED ARROW".

OLA IS A "STEADY RIGHT-TURN GREEN ARROW" DISPLAY.

BARRIER

CONTROLLER LOGIC

PHASE NUMBER	PHASE LOCKING	DUAL ENTRY W/Ø	PHASE RECALL	PHASE ACTIVE
1				
2	X	6	MIN	X
3				
4				
5		2		X
6	X	2	MIN	X
7				X
8				X

OVERLAPS

O.L. "A" = 5,8
 O.L. "B" = 6
 O.L. "C" =
 O.L. "D" =

TYPE OF INTERCONNECT/COMMUNICATION	
NONE	
CLOSED LOOP	
TWISTED PAIR	
FIBER OPTIC*	
FIBER OPTIC (ETHERNET)	
RADIO	
CELL MODEM	X

TYPE OF COORDINATION	
NONE	X
TBC	
TRAFFIC RESPONSIVE	
ADAPTIVE	
*LOCATION OF MASTER CONTROLLER NO:	S-
SIGNAL SYSTEM NO:	SS-

TYPE OF LIGHTING	
BY OTHER AGENCY	
IN TRAFFIC CABINET	X
IN SEPARATE DOT LIGHTING CABINET	

TYPE OF PRE-EMPT	
NONE	X
RAILROAD	
EMERGENCY VEHICLE	
GTT	
TOMAR	
HARDWARE	
OTHER	
LIFT BRIDGE	
QUEUE DETECTION	

DETECTOR LOGIC

* EXISTING LOOP #71 IS NOT CURRENTLY BEING UTILIZED.

DETECTOR INPUT	3	1	7	5	11	9	15	13
PLAN LOOP DETECTORS*(S)	61	21	51	*71	81	83	OL-1	
CALLED PHASE	6	2	5			8	8	
CALL OPTION	X	X	X			X	X	
DELAY TIME							8.0	
EXTENSION OPTION	X	X	X		X	X	X	
EXTEND TIME	3.5	3.5	3.0		2.0	2.0	2.0	
USE ADDED INITIAL								
CROSS SWITCH PHASE								
STRETCH TIME					2.0			
LOOP SIZE (FTxFT)	6x6	6x6	6x30	6x6	6x30	6x20		
NUMBER TURNS	4	4	3	4	4	3	3	

DETECTOR INPUT	19	17	23	21	27	25	31	29
PLAN LOOP DETECTORS*(S)								
CALLED PHASE								
CALL OPTION								
DELAY TIME								
EXTENSION OPTION								
EXTEND TIME								
USE ADDED INITIAL								
CROSS SWITCH PHASE								
STRETCH TIME								
LOOP SIZE (FTxFT)								
NUMBER TURNS								

DETECTOR INPUT	19	17	23	21	27	25	31	29
PLAN LOOP DETECTORS*(S)								
CALLED PHASE								
CALL OPTION								
DELAY TIME								
EXTENSION OPTION								
EXTEND TIME								
USE ADDED INITIAL								
CROSS SWITCH PHASE								
STRETCH TIME								
LOOP SIZE (FTxFT)								
NUMBER TURNS								

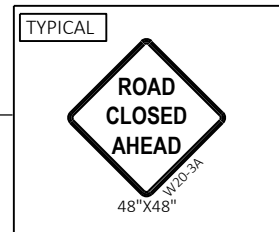
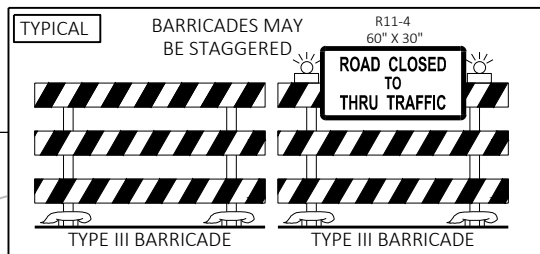
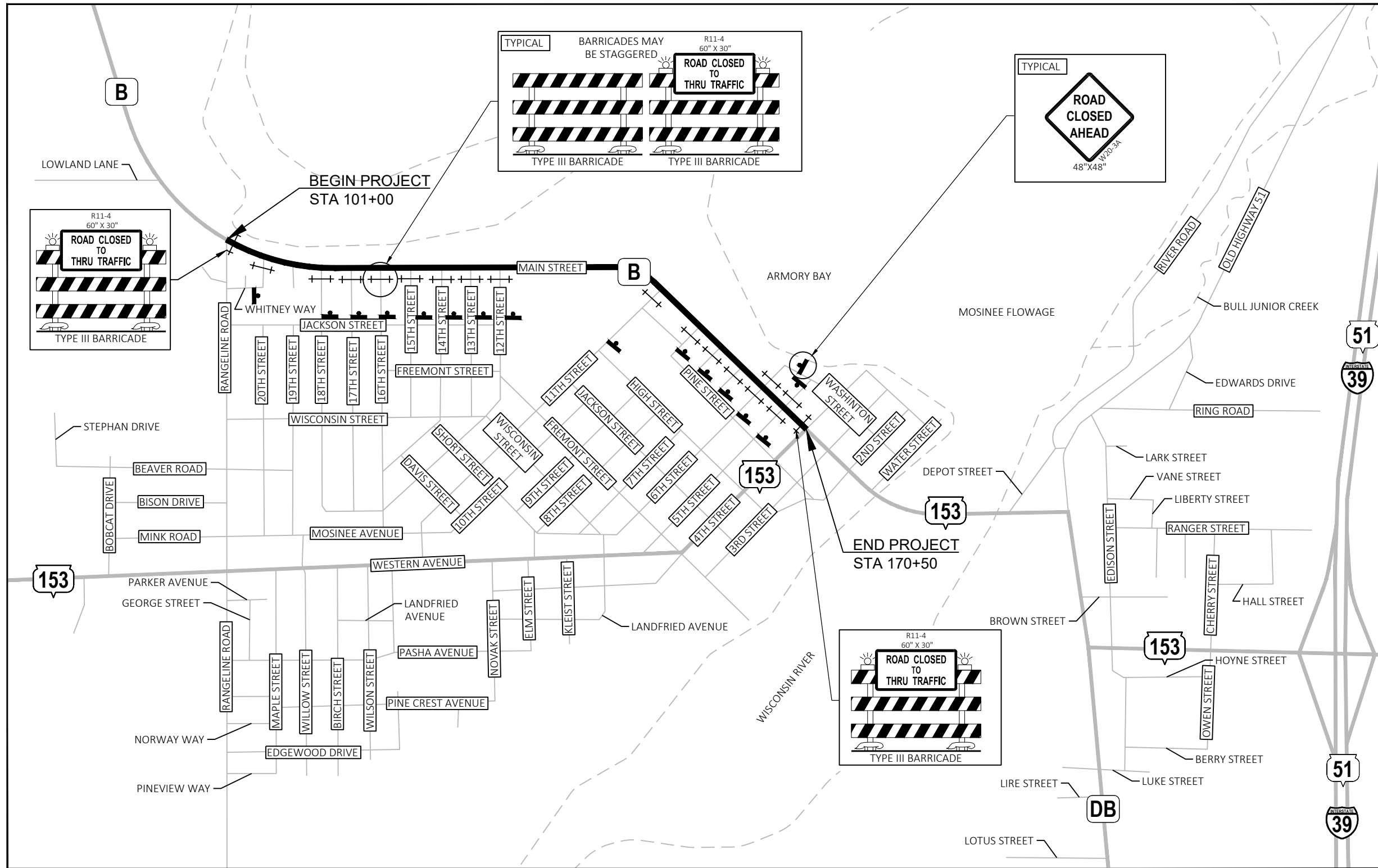
DETECTOR INPUT	4	2	8	6	12	10	16	14
PLAN LOOP DETECTORS*(S)		22		72	82		OL-2	
CALLED PHASE		2		7			8	
CALL OPTION		X		X			X	
DELAY TIME				6.0			8.0	
EXTENSION OPTION		X		X	X		X	
EXTEND TIME		3.5		3.0	2.0		2.0	
USE ADDED INITIAL								
CROSS SWITCH PHASE								
STRETCH TIME					2.0			
LOOP SIZE (FTxFT)		6x6		6x30	6x6		6x20	
NUMBER TURNS		4		3	4		3	

DETECTOR INPUT	20	18	24	22	28	26	32	30
PLAN LOOP DETECTORS*(S)								
CALLED PHASE								
CALL OPTION								
DELAY TIME								
EXTENSION OPTION								
EXTEND TIME								
USE ADDED INITIAL								
CROSS SWITCH PHASE								
STRETCH TIME								
LOOP SIZE (FTxFT)								
NUMBER TURNS								

DETECTOR INPUT	20	18	24	22	28	26	32	30
PLAN LOOP DETECTORS*(S)								
CALLED PHASE								
CALL OPTION								
DELAY TIME								
EXTENSION OPTION								
EXTEND TIME								
USE ADDED INITIAL								
CROSS SWITCH PHASE								
STRETCH TIME								
LOOP SIZE (FTxFT)								
NUMBER TURNS								

GENERAL NOTES

- ANY ACTUATED PHASE FOR WHICH THERE IS NO CALL SHALL BE SKIPPED.
- WHEN ONE PHASE IS ON ALONE, ANY NON-CONFLICTING PHASE MAY START TIMING CONCURRENTLY WITHOUT A CLEARANCE INTERVAL.
- WHEN OPPOSING THROUGH PHASES ARE TIMING CONCURRENTLY, THEY SHALL TERMINATE TOGETHER DUE TO PERMISSIVE LEFT TURN CONFLICT.
- A "STEADY RIGHT-TURN YELLOW ARROW" SIGNAL INDICATION SHALL NOT BE DISPLAYED BETWEEN THE DISPLAY OF THE "FLASHING RIGHT-TURN YELLOW ARROW" SIGNAL INDICATION AND THE DISPLAY OF THE "STEADY RIGHT-TURN GREEN ARROW" SIGNAL INDICATION.

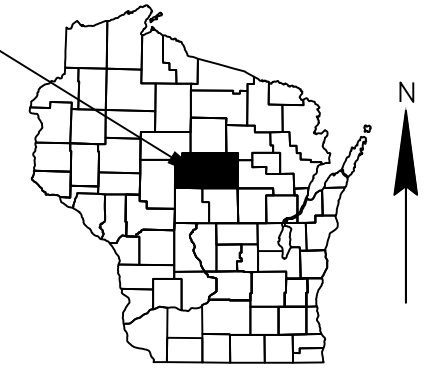


NOTES:

MAIN STREET / SIDE STREET CLOSURES TO BE IN PLACE FOR THE ROADWAY PULVERIZE AND RELAY WORK

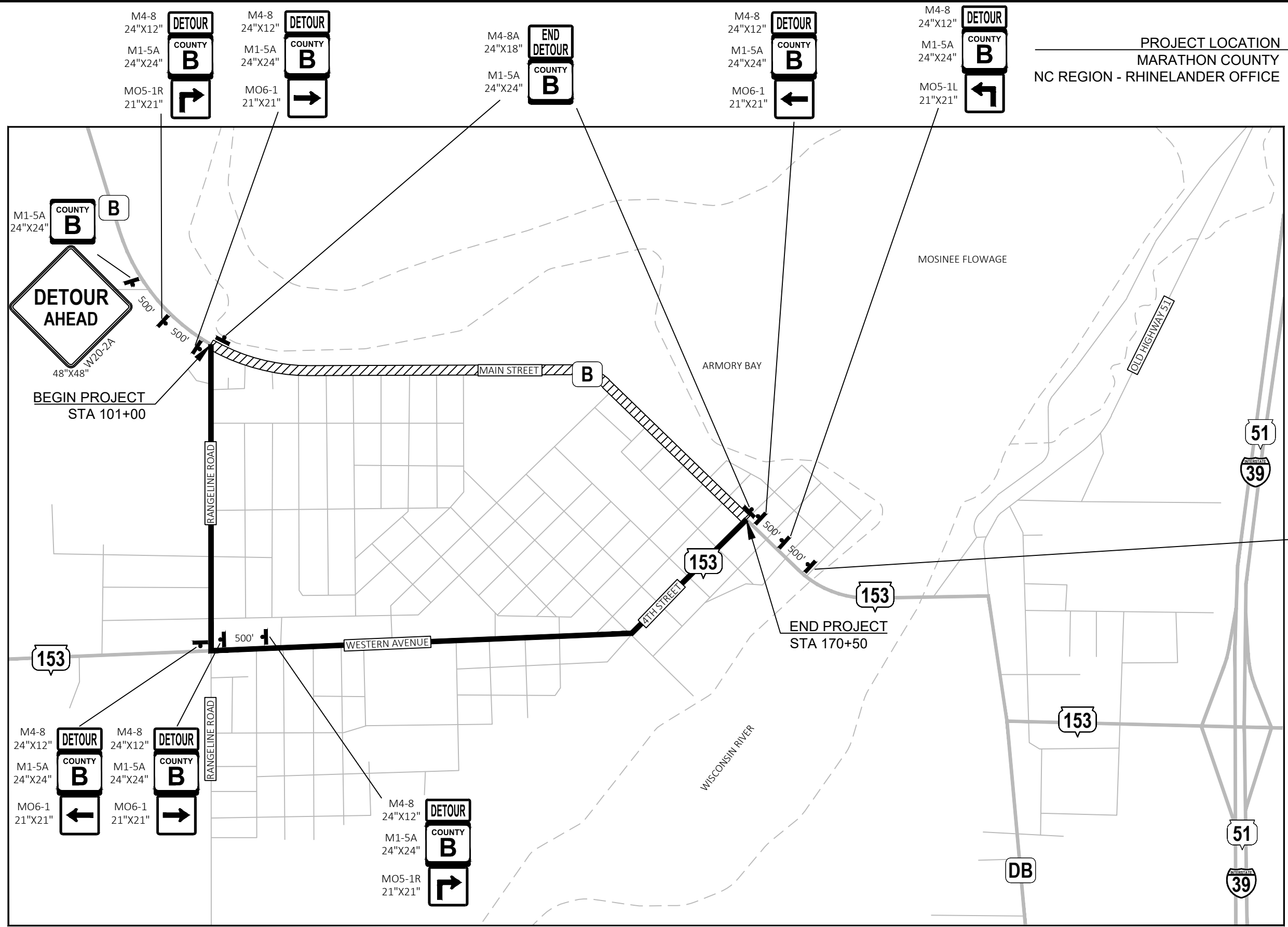
SEE DETOUR PLANS FOR MORE INFORMATION

PROJECT LOCATION
MARATHON COUNTY
NC REGION - RHINELANDER OFFICE



LEGEND

- ALTERNATE ROUTE (represented by a solid black line)
- WORK ZONE (represented by a hatched area)



M4-8 24"x12" **DETOUR**
 M1-5A 24"x24" **COUNTY B**
 MO5-1R 21"x21" **→**

M4-8 24"x12" **DETOUR**
 M1-5A 24"x24" **COUNTY B**
 MO6-1 21"x21" **→**

M4-8A 24"x18" **END**
DETOUR
COUNTY B

M4-8 24"x12" **DETOUR**
 M1-5A 24"x24" **COUNTY B**
 MO6-1 21"x21" **←**

M4-8 24"x12" **DETOUR**
 M1-5A 24"x24" **COUNTY B**
 MO5-1L 21"x21" **↙**

M1-5A 24"x24" **COUNTY B**
DETOUR AHEAD
 48"x48" W20-2A
 BEGIN PROJECT
 STA 101+00

M1-5A 24"x24" **COUNTY B**
DETOUR AHEAD
 48"x48" W20-2A

M4-8 24"x12" **DETOUR**
 M1-5A 24"x24" **COUNTY B**
 MO6-1 21"x21" **←**

M4-8 24"x12" **DETOUR**
 M1-5A 24"x24" **COUNTY B**
 MO6-1 21"x21" **→**

M4-8 24"x12" **DETOUR**
 M1-5A 24"x24" **COUNTY B**
 MO5-1R 21"x21" **↗**

Estimate Of Quantities

6999-16-73

Line	Item	Item Description	Unit	Total	Qty
0002	204.0115	Removing Asphaltic Surface Butt Joints	SY	795.000	795.000
0004	204.0120	Removing Asphaltic Surface Milling	SY	1,050.000	1,050.000
0006	204.0150	Removing Curb & Gutter	LF	4,100.000	4,100.000
0008	204.0155	Removing Concrete Sidewalk	SY	865.000	865.000
0010	204.0220	Removing Inlets	EACH	1.000	1.000
0012	204.9180.S	Removing (item description) 01. Excess Pulverized Material	SY	27,350.000	27,350.000
0014	211.0101	Prepare Foundation for Asphaltic Paving (project) 01. 6999-16-73	EACH	1.000	1.000
0016	213.0100	Finishing Roadway (project) 01. 6999-16-73	EACH	1.000	1.000
0018	305.0110	Base Aggregate Dense 3/4-Inch	TON	200.000	200.000
0020	305.0120	Base Aggregate Dense 1 1/4-Inch	TON	200.000	200.000
0022	325.0100	Pulverize and Relay	SY	27,350.000	27,350.000
0024	405.1000	Stamping Colored Concrete	CY	1.000	1.000
0026	455.0605	Tack Coat	GAL	1,400.000	1,400.000
0028	460.2000	Incentive Density HMA Pavement	DOL	3,140.000	3,140.000
0030	460.5224	HMA Pavement 4 LT 58-28 S	TON	4,900.000	4,900.000
0032	601.0411	Concrete Curb & Gutter 30-Inch Type D	LF	4,100.000	4,100.000
0034	601.0600	Concrete Curb Pedestrian	LF	130.000	130.000
0036	602.0405	Concrete Sidewalk 4-Inch	SF	2,400.000	2,400.000
0038	602.0415	Concrete Sidewalk 6-Inch	SF	4,225.000	4,225.000
0040	602.0515	Curb Ramp Detectable Warning Field Natural Patina	SF	250.000	250.000
0042	602.0615	Curb Ramp Detectable Warning Field Radial Natural Patina	SF	59.000	59.000
0044	602.0810	Concrete Driveway 6-Inch	SY	40.000	40.000
0046	611.0430	Reconstructing Inlets	EACH	6.000	6.000
0048	611.8110	Adjusting Manhole Covers	EACH	12.000	12.000
0050	611.8115	Adjusting Inlet Covers	EACH	17.000	17.000
0052	618.0100	Maintenance and Repair of Haul Roads (project) 01. 6999-16-73	EACH	1.000	1.000
0054	619.1000	Mobilization	EACH	1.000	1.000
0056	624.0100	Water	MGAL	50.000	50.000
0058	625.0100	Topsoil	SY	640.000	640.000
0060	628.1905	Mobilizations Erosion Control	EACH	1.000	1.000
0062	628.1910	Mobilizations Emergency Erosion Control	EACH	2.000	2.000
0064	628.2006	Erosion Mat Urban Class I Type A	SY	640.000	640.000
0066	628.7005	Inlet Protection Type A	EACH	35.000	35.000
0068	628.7015	Inlet Protection Type C	EACH	35.000	35.000
0070	629.0210	Fertilizer Type B	CWT	0.400	0.400
0072	630.0140	Seeding Mixture No. 40	LB	17.200	17.200
0074	630.0500	Seed Water	MGAL	16.000	16.000
0076	642.5201	Field Office Type C	EACH	1.000	1.000
0078	643.0300	Traffic Control Drums	DAY	7,500.000	7,500.000
0080	643.0410	Traffic Control Barricades Type II	DAY	500.000	500.000
0082	643.0420	Traffic Control Barricades Type III	DAY	1,000.000	1,000.000
0084	643.0705	Traffic Control Warning Lights Type A	DAY	1,050.000	1,050.000
0086	643.0900	Traffic Control Signs	DAY	4,350.000	4,350.000
0088	643.1070	Traffic Control Cones 42-Inch	DAY	11,700.000	11,700.000
0090	643.5000	Traffic Control	EACH	1.000	1.000
0092	644.1410	Temporary Pedestrian Surface Asphalt	SF	500.000	500.000
0094	644.1601	Temporary Pedestrian Curb Ramp	DAY	280.000	280.000
0096	644.1605	Temporary Pedestrian Detectable Warning Field	SF	200.000	200.000
0098	644.1810	Temporary Pedestrian Barricade	LF	885.000	885.000
0100	646.1020	Marking Line Epoxy 4-Inch	LF	10,750.000	10,750.000

Estimate Of Quantities

6999-16-73

Line	Item	Item Description	Unit	Total	Qty
0102	646.3020	Marking Line Epoxy 8-Inch	LF	550.000	550.000
0104	646.5020	Marking Arrow Epoxy	EACH	18.000	18.000
0106	646.6020	Marking Stop Line Epoxy 12-Inch	LF	300.000	300.000
0108	646.7020	Marking Diagonal Epoxy 6-Inch	LF	300.000	300.000
0110	646.7420	Marking Crosswalk Epoxy Transverse Line 6-Inch	LF	1,200.000	1,200.000
0112	646.8320	Marking Parking Stall Epoxy	LF	200.000	200.000
0114	650.5500	Construction Staking Curb Gutter and Curb & Gutter	LF	4,100.000	4,100.000
0116	650.8000	Construction Staking Resurfacing Reference	LF	6,950.000	6,950.000
0118	650.9000	Construction Staking Curb Ramps	EACH	30.000	30.000
0120	650.9911	Construction Staking Supplemental Control (project) 01. 6999-16-73	EACH	1.000	1.000
0122	690.0150	Sawing Asphalt	LF	4,200.000	4,200.000
0124	690.0250	Sawing Concrete	LF	520.000	520.000
0126	ASP.1T0A	On-the-Job Training Apprentice at \$5.00/HR	HRS	300.000	300.000
0128	ASP.1T0G	On-the-Job Training Graduate at \$5.00/HR	HRS	600.000	600.000
0130	SPV.0060	Special 01. Adjusting Sanitary Manholes	EACH	17.000	17.000
0132	SPV.0060	Special 02. Adjusting Water Valve Boxes	EACH	13.000	13.000
0134	SPV.0060	Special 03. Adjusting Water Manholes	EACH	3.000	3.000

3

REMOVING ASPHALTIC SURFACE BUTT JOINTS

STATION - STATION	LOCATION	204.0115 SY	COMMENTS
CATEGORY CODE 0010			
101+00	BEGIN	94	
107+86	OAK PARK CIR	38	
111+08	18TH ST	37	
114+29	17TH ST	37	
117+54	16TH ST	37	
120+84	15TH ST	37	
124+05	14TH ST	36	
127+31	13TH ST	37	
130+56	12TH ST	36	
148+89	11TH ST	37	
154+82	9TH ST	38	
158+04	8TH ST	37	
161+24	7TH ST	38	
164+43	6TH ST (LT)	38	
164+44	6TH ST (RT)	38	
167+67	5TH ST (LT)	37	
167+68	5TH ST (RT)	37	
170+50	END	106	
TOTAL		795	

REMOVING PAVEMENT ITEMS

STATION - STATION	LOCATION	204.0120 REMOVING ASPHALTIC SURFACE MILLING SY	325.0100 PULVERIZE AND RELAY SY	COMMENTS
CATEGORY CODE 0010				
101+00 - 168+00	CL	--	27,350	3" MILL
168+00 - 170+50	CL	1,050	--	3" MILL/FILL
TOTAL		1,050	27,350	

REMOVING CONCRETE SIDEWALK

STATION - STATION	LOCATION	204.0155 SY	COMMENTS
CATEGORY CODE 0010			
101+00 - 170+50	LT/RT	465	CURB RAMP REPLACEMENTS
101+00 - 170+50	LT/RT	400	SPOT REPLACEMENTS
TOTAL		865	

REMOVING CURB AND GUTTER

STATION - STATION	LOCATION	204.0150 LF	COMMENTS
CATEGORY CODE 0010			
101+00 - 170+50	LT/RT	480	CURB RAMP REPLACEMENTS
101+00 - 170+50	LT/RT	3,620	SPOT REPLACEMENTS
TOTAL		4,100	

BASE AGGREGATE DENSE

STATION - STATION	305.0110 BASE AGGREGATE DENSE 3/4-INCH TON	305.0120 BASE AGGREGATE DENSE 1 1/4-INCH TON	COMMENTS
CATEGORY CODE 0010			
110+00 - 170+50	200	--	
110+00 - 170+50	--	200	
TOTALS		200	200

ASPHALTIC ITEMS

STATION - STATION	LOCATION	455.0605 TACK COAT GAL	460.5224 HMA PAVEMENT 4LT 58-28S TON	COMMENTS
CATEGORY CODE 0010				
101+00 - 170+50	CL	1,400	4,900	
TOTALS		1,400	4,900	

CONCRETE SIDEWALK ITEMS

STATION - STATION	LOCATION	405.1000 STAMPING COLORED CONCRETE CY	601.0600 CONCRETE CURB PEDESTRIAN LF	602.0405 CONCRETE SIDEWALK 4-INCH SF	602.0415 CONCRETE SIDEWALK 6-INCH SF	602.0515 CURB RAMP DETECTABLE WARNING FIELD NATURAL PATINA SF	602.0615 CURB RAMP WARNING FIELD RADIAL NATURAL PATINA SF	COMMENTS
CATEGORY CODE 0010								
101+00 - 170+50	LT/RT	--	130	2,000	4,225	250	59	CURB RAMP REPLACEMENTS
101+00 - 170+50	LT/RT	--	--	400	--	--	--	SPOT REPLACEMENTS
168+02 - 168+22	RT	1	--	--	--	--	--	COLORED CONCRETE REPLACEMENT
TOTALS		1	130	2,400	4,225	250	59	

STRUCTURE ITEMS

STATION	LOCATION	204.0220 REMOVING INLETS EACH	611.0430 RECONSTRUCTING INLETS EACH	611.8110 ADJUSTING MANHOLE COVERS EACH	611.8115 ADJUSTING INLET COVERS EACH	SPV.0060.01 ADJUSTING SANITARY MANHOLES EACH	SPV.0060.02 ADJUSTING WATER VALVE BOXES EACH	SPV.0060.03 ADJUSTING WATER MANHOLES EACH
CATEGORY CODE 0010								
101+00 - 170+50	LT/RT/CL	1	6	12	17	--	--	--
CATEGORY CODE 0020								
101+00 - 170+50	LT/RT/CL	--	--	--	--	17	13	3
TOTALS		1	6	12	17	17	13	3

ANCILLARY CONCRETE ITEMS

STATION - STATION	LOCATION	601.0411 CONCRETE CURB & GUTTER 30-INCH TYPE D LF	602.0810 CONCRETE DRIVEWAY 6-INCH SY	COMMENTS
CATEGORY CODE 0010				
101+00 - 170+50	LT/RT	480	--	CURB RAMP REPLACEMENTS
101+00 - 170+50	LT/RT	3,620	--	SPOT REPLACEMENTS
101+00 - 170+50	LT/RT	--	40	
TOTALS		4,100	40	

MISC. SHEET 1

3

LANDSCAPING ITEMS

STATION - STATION	LOCATION	625.0100 TOPSOIL SY	628.2006 EROSION MAT URBAN CLASS I TYPE A SY	629.0210 FERTILIZER TYPE B CWT	630.0140 SEED MIX NO. 40 LBS	630.0500 SEED WATER MGAL	COMMENTS
CATEGORY CODE 0010							
101+00 - 170+50	LT/RT	455	455	0.28	12.3	11	CURB RAMP REPLACEMENTS
101+00 - 170+50	LT/RT	165	165	0.11	4.4	4	SPOT CURB REPLACEMENTS
101+00 - 170+50	LT/RT	20	20	0.01	0.5	1	SPOT SIDEWALK REPLACEMENTS
TOTALS		640	640	0.40	17.2	16	

TEMPORARY PEDESTRIAN ITEMS

LOCATION	644.1410 TEMP PED SURFACE ASPHALT SF	644.1601 TEMP PED CURB RAMP DAYS	644.1605 TEMP PED DETECTABLE WARNING FIELD SF	644.1810 TEMP PED BARRICADE LF
CATEGORY CODE 0010				
101+00 - 170+50	500	280	200	885
TOTALS		500	280	885

3

SAWING ITEMS

LOCATION	690.0150 SAWING ASPHALT LF	690.0250 SAWING CONCRETE LF	COMMENTS
CATEGORY CODE 0010			
101+00 - 170+50	100	--	UNDISTRIBUTED
101+00 - 170+50	--	520	CURB RAMPS / SPOT REPLACEMENTS
101+00 - 170+50	4,100	--	CURB RAMPS / SPOT REPLACEMENTS
TOTALS		4,200	520

PAVEMENT MARKING

LOCATION	OFFSET	646.1020 MARKING LINE EPOXY 4-INCH YELLOW LF	646.3020 MARKING LINE EPOXY 8-INCH WHITE LF	646.5020 MARKING ARROWS EPOXY EACH	646.6020 MARKING STOP LINE EPOXY 12-INCH LF	646.7020 MARKING DIAGONAL EPOXY 6-INCH LF	646.7420 MARKING CROSSWALK EPOXY TRANSVERSE LINE 6-INCH LF	646.8320 MARKING PARKING STALL EPOXY LF
CATEGORY CODE 0010								
101+00 - 170+50	LT/RT/CL	10,750	550	18	300	300	1,200	200
TOTALS		10,750	550	18	300	300	1,200	200

REMOVING EXCESS PULVERIZED MATERIAL

STATION - STATION	LOCATION	204.9180.S SY
CATEGORY CODE 0010		
101+00 - 168+00	CL	27,350
TOTAL		27,350

CONSTRUCTION STAKING ITEMS

STATION - STATION	LOCATION	650.5500 CURB GUTTER CURB & GUTTER LF	650.8000 RESURFACING REFERENCE LF	650.9000 CURB RAMPS LS	650.9911 SUPPLEMENTAL CONTROL EACH
CATEGORY CODE 0010					
101+00 - 170+50	LT/RT/CL	4,100	6,950	30	1
TOTALS		4,100	6,950	30	1

WATER

STATION	LOCATION	624.0100 MGAL
CATEGORY CODE 0010		
101+00 - 170+50	PROJECT	50
TOTALS		50

PREPARE FOUNDATION FOR ASPHALTIC PAVING

STATION	LOCATION	211.0101 EACH
CATEGORY CODE 0010		
101+00 - 170+50	PROJECT	1
TOTALS		1

EROSION CONTROL ITEMS

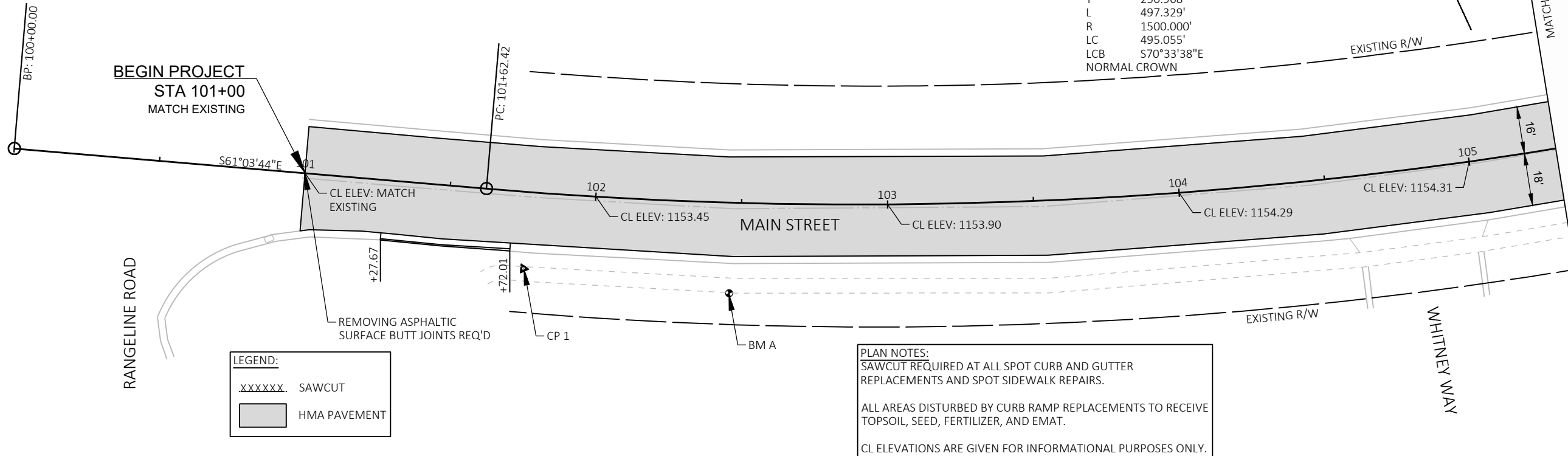
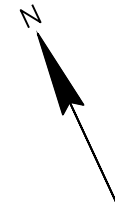
STATION	LOCATION	628.1905 MOBILIZATIONS EROSION CONTROL EACH	628.1910 MOBILIZATIONS EMERGENCY EROSION CONTROL EACH	628.7005 INLET PROTECTION TYPE A EACH	628.7015 INLET PROTECTION TYPE C EACH
CATEGORY CODE 0010					
101+00 - 170+50	PROJECT	1	2	35	35
SUBTOTALS		1	2	35	35

TRAFFIC CONTROL ITEMS

LOCATION	643.0300 DRUMS DAYS	643.0410 TRAFFIC CONTROL BARRICADES TYPE II DAYS	643.0420 TRAFFIC CONTROL BARRICADES TYPE III DAYS	643.0705 TRAFFIC CONTROL WARNING LIGHTS TYPE A DAYS	643.0900 SIGNS DAYS	643.1070 TRAFFIC CONTROL CONES 42-INCH DAYS
CATEGORY CODE 0010						
101+00 - 170+50	7,500	500	1,000	1,050	4,350	11,700
TOTALS		7,500	500	1,000	4,350	11,700

MISC. SHEET 2

CL CURVE DATA
 CURVE 1
 PI 104+13.388
 N 143108.167
 E 256416.769
 DELTA 18°59'48"
 D 03°49'11"
 T 250.968'
 L 497.329'
 R 1500.000'
 LC 495.055'
 LCB S70°33'38"E
 NORMAL CROWN



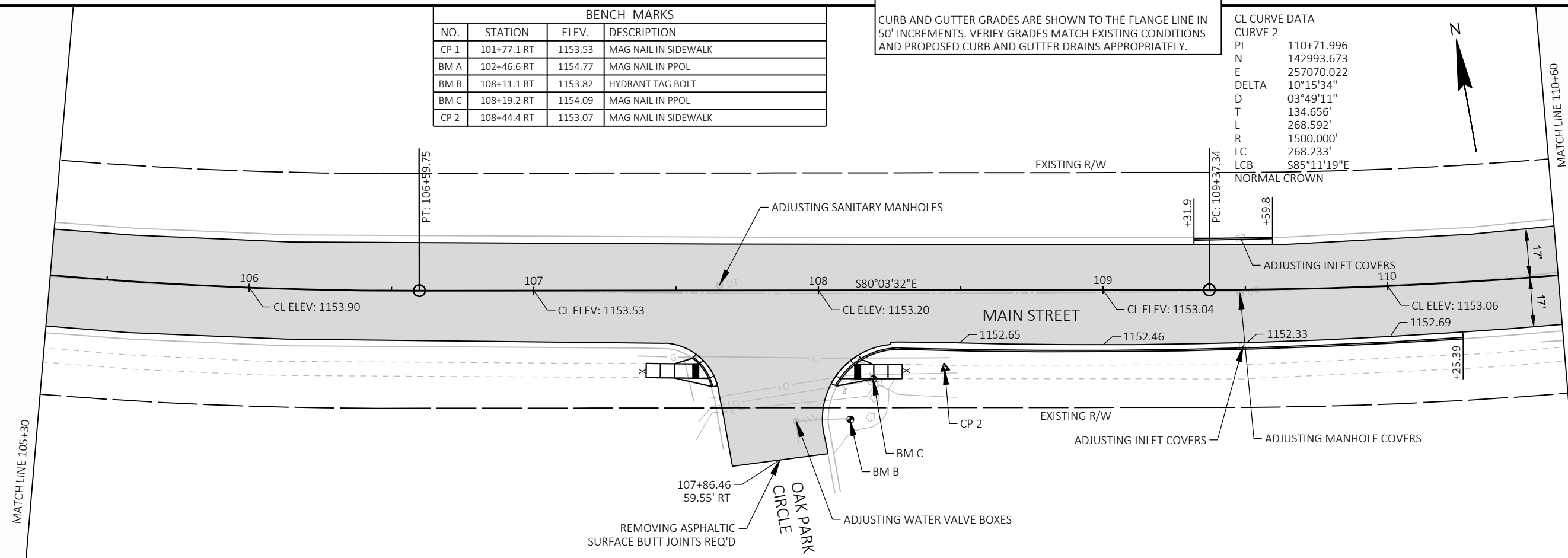
LEGEND:

xxxxxx	SAWCUT
[Shaded Box]	HMA PAVEMENT

PLAN NOTES:
 SAWCUT REQUIRED AT ALL SPOT CURB AND GUTTER REPLACEMENTS AND SPOT SIDEWALK REPAIRS.
 ALL AREAS DISTURBED BY CURB RAMP REPLACEMENTS TO RECEIVE TOPSOIL, SEED, FERTILIZER, AND EMAT.
 CL ELEVATIONS ARE GIVEN FOR INFORMATIONAL PURPOSES ONLY.
 CURB AND GUTTER GRADES ARE SHOWN TO THE FLANGE LINE IN 50' INCREMENTS. VERIFY GRADES MATCH EXISTING CONDITIONS AND PROPOSED CURB AND GUTTER DRAINS APPROPRIATELY.

BENCH MARKS			
NO.	STATION	ELEV.	DESCRIPTION
CP 1	101+77.1 RT	1153.53	MAG NAIL IN SIDEWALK
BM A	102+46.6 RT	1154.77	MAG NAIL IN PPOL
BM B	108+11.1 RT	1153.82	HYDRANT TAG BOLT
BM C	108+19.2 RT	1154.09	MAG NAIL IN PPOL
CP 2	108+44.4 RT	1153.07	MAG NAIL IN SIDEWALK

CL CURVE DATA
 CURVE 2
 PI 110+71.996
 N 142993.673
 E 257070.022
 DELTA 10°15'34"
 D 03°49'11"
 T 134.656'
 L 268.592'
 R 1500.000'
 LC 268.233'
 LCB S85°11'19"E
 NORMAL CROWN



BENCH MARKS			
NO.	STATION	ELEV.	DESCRIPTION
BM D	110+87.4 RT	1153.88	MAG NAIL IN PPOL
BM E	111+30.0 RT	1154.76	HYDRANT TAG BOLT
BM F	114+05.8 RT	1154.37	MAG NAIL IN PPOL
BM G	114+68.5 RT	1154.73	HYDRANT TAG BOLT

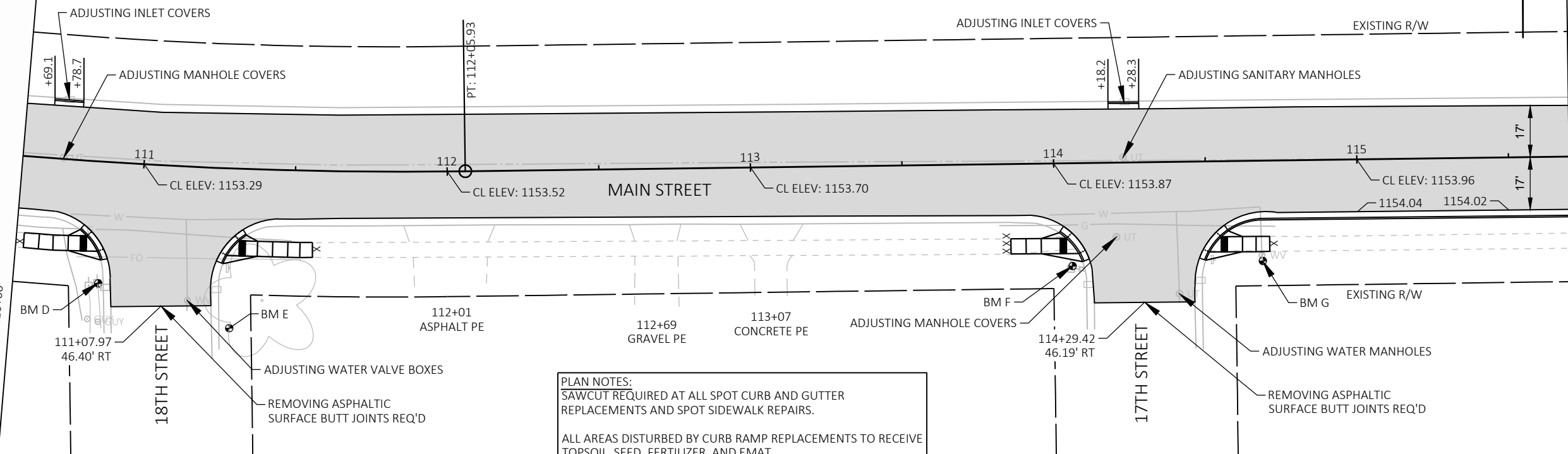
LEGEND:

XXXXXX SAWCUT

[Grey Box] HMA PAVEMENT



MATCH LINE 115+70



5

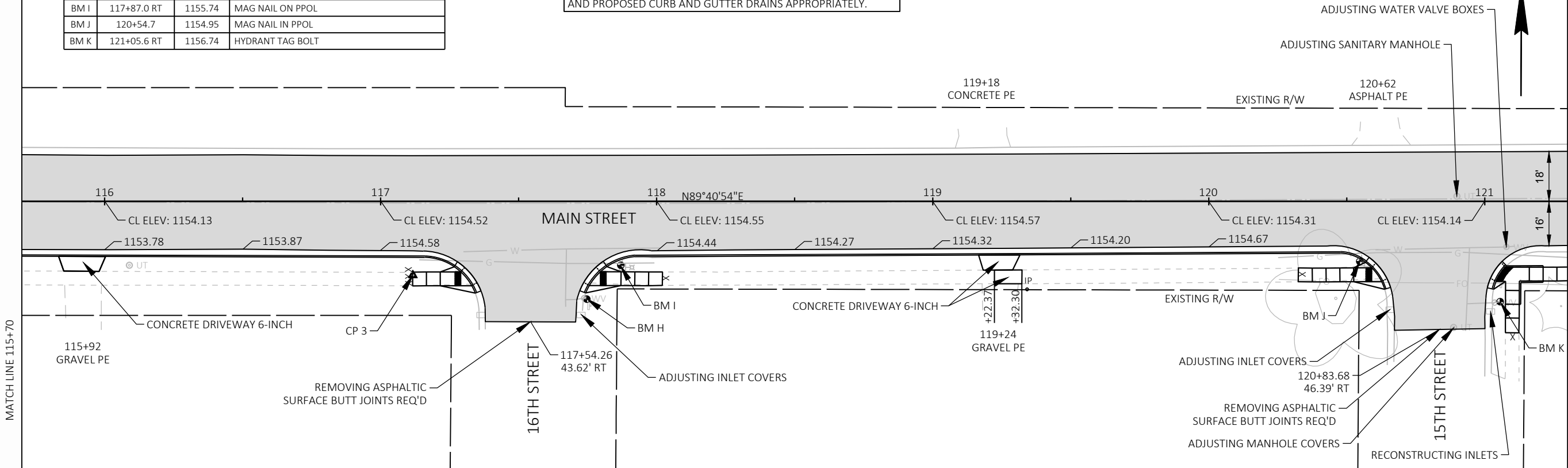
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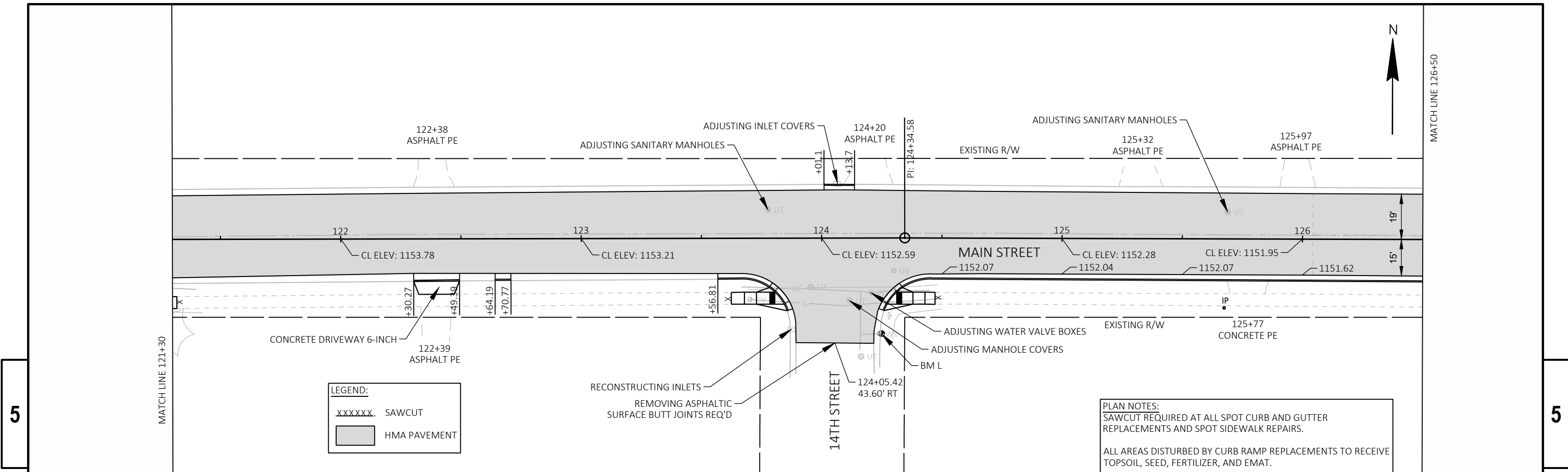
PLAN NOTES:
 SAWCUT REQUIRED AT ALL SPOT CURB AND GUTTER REPLACEMENTS AND SPOT SIDEWALK REPAIRS.
 ALL AREAS DISTURBED BY CURB RAMP REPLACEMENTS TO RECEIVE TOPSOIL, SEED, FERTILIZER, AND EMAT.
 CL ELEVATIONS ARE GIVEN FOR INFORMATIONAL PURPOSES ONLY.
 CURB AND GUTTER GRADES ARE SHOWN TO THE FLANGE LINE IN 50' INCREMENTS. VERIFY GRADES MATCH EXISTING CONDITIONS AND PROPOSED CURB AND GUTTER DRAINS APPROPRIATELY.

BENCH MARKS			
NO.	STATION	ELEV.	DESCRIPTION
CP 3	117+11.7 RT	1154.62	MAG NAIL IN SIDEWALK
BM H	117+74.7 RT	1156.49	HYDRANT ARROW
BM I	117+87.0 RT	1155.74	MAG NAIL ON PPOL
BM J	120+54.7	1154.95	MAG NAIL IN PPOL
BM K	121+05.6 RT	1156.74	HYDRANT TAG BOLT



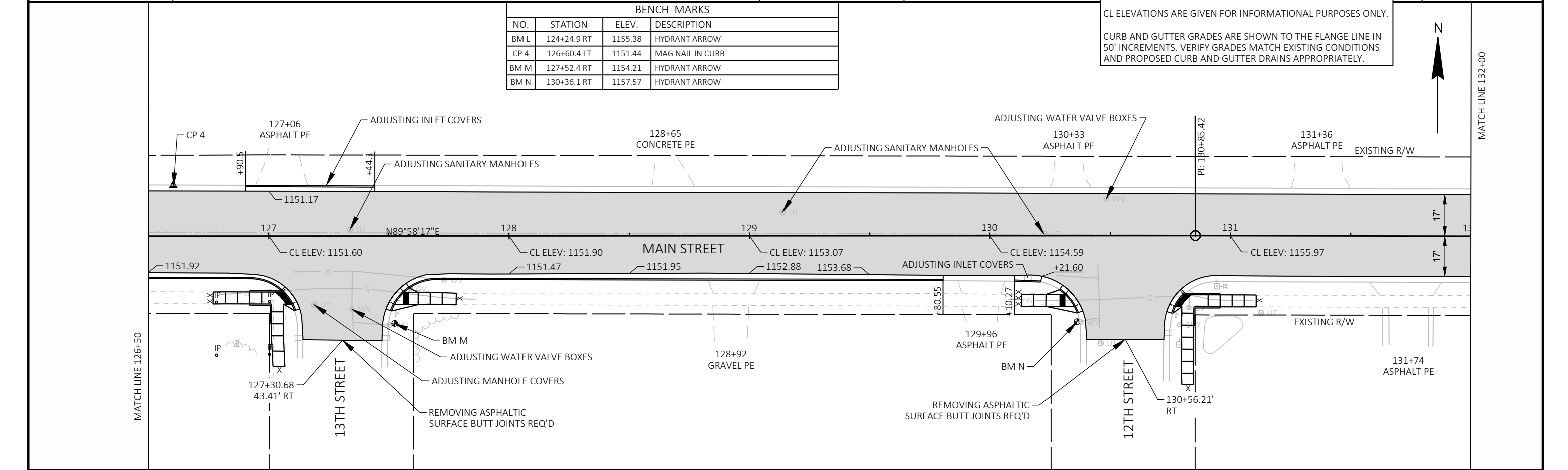
MATCH LINE 121+30

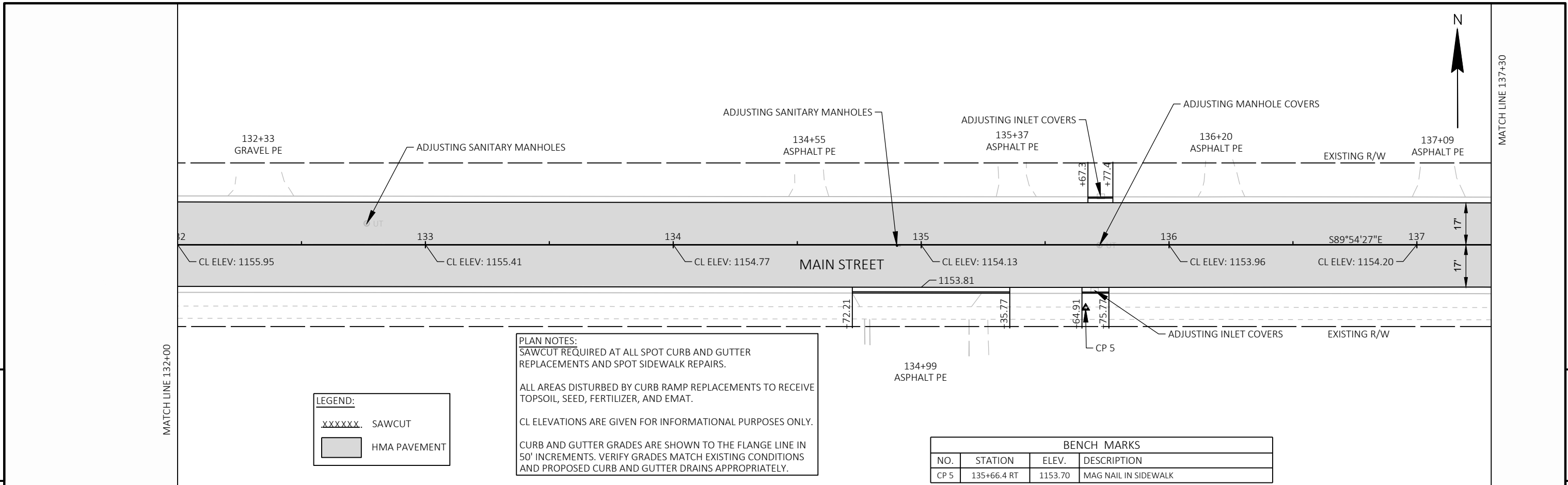




BENCH MARKS			
NO.	STATION	ELEV.	DESCRIPTION
BM L	124+24.9 RT	1155.38	HYDRANT ARROW
CP 4	126+60.4 LT	1151.44	MAG NAIL IN CURB
BM M	127+52.4 RT	1154.21	HYDRANT ARROW
BM N	130+36.1 RT	1157.57	HYDRANT ARROW

PLAN NOTES:
 SAWCUT REQUIRED AT ALL SPOT CURB AND GUTTER REPLACEMENTS AND SPOT SIDEWALK REPAIRS.
 ALL AREAS DISTURBED BY CURB RAMP REPLACEMENTS TO RECEIVE TOPSOIL, SEED, FERTILIZER, AND EMAT.
 CL ELEVATIONS ARE GIVEN FOR INFORMATIONAL PURPOSES ONLY.
 CURB AND GUTTER GRADES ARE SHOWN TO THE FLANGE LINE IN 50' INCREMENTS. VERIFY GRADES MATCH EXISTING CONDITIONS AND PROPOSED CURB AND GUTTER DRAINS APPROPRIATELY.



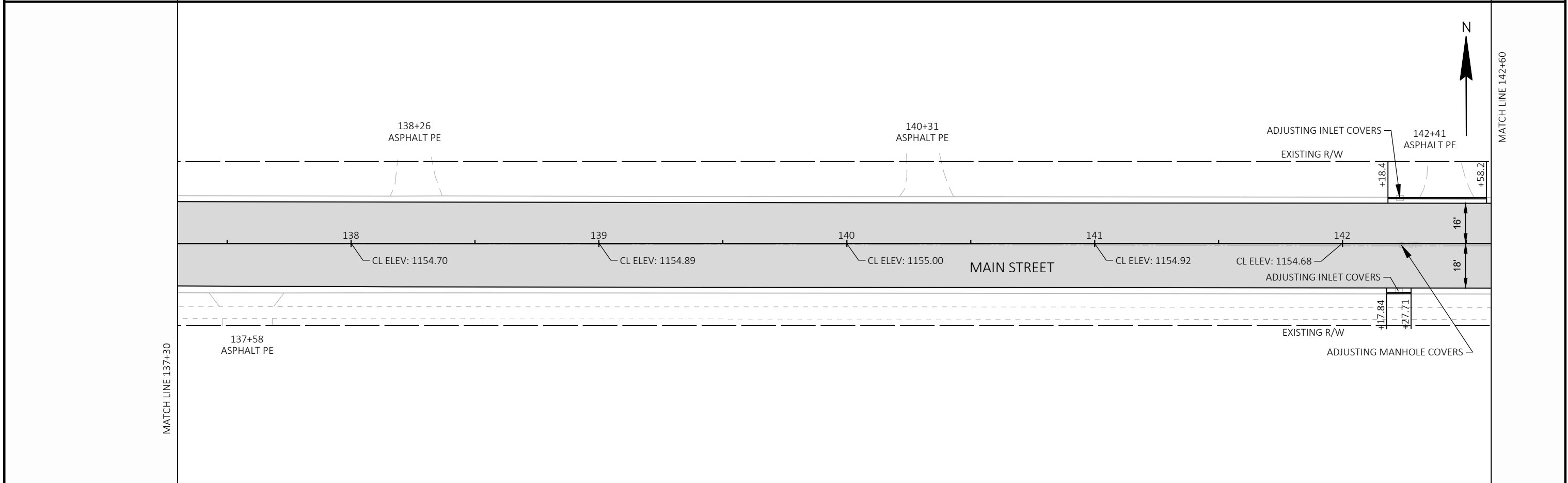


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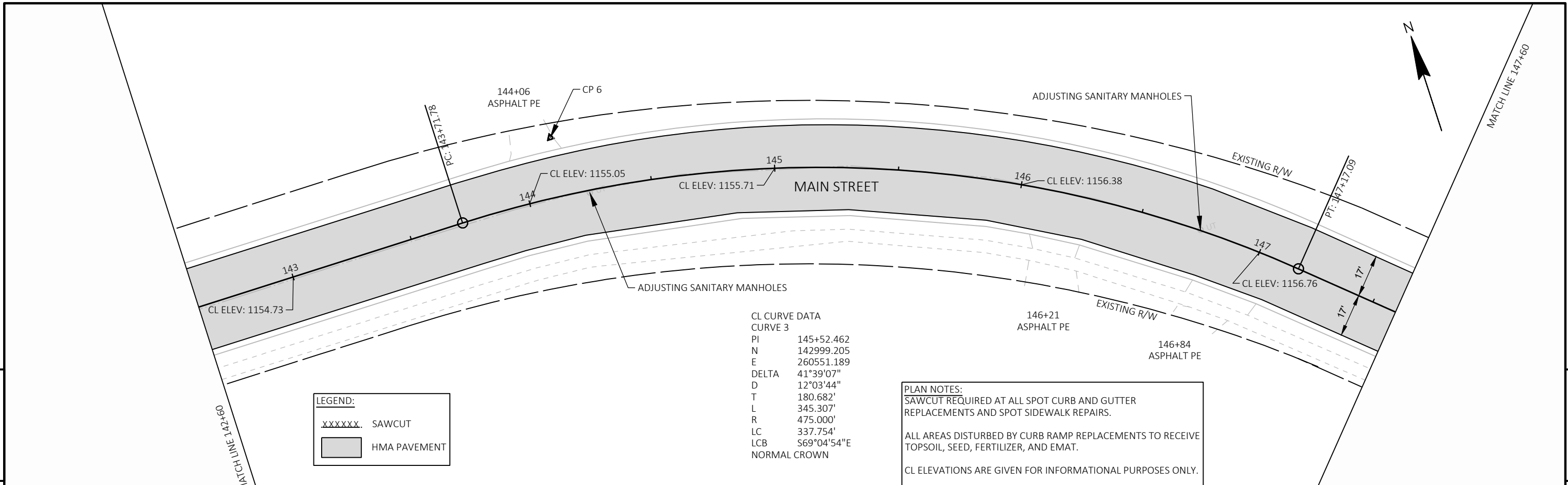
xxxxxx	SAWCUT
[Hatched Box]	HMA PAVEMENT

PLAN NOTES:
 SAWCUT REQUIRED AT ALL SPOT CURB AND GUTTER REPLACEMENTS AND SPOT SIDEWALK REPAIRS.
 ALL AREAS DISTURBED BY CURB RAMP REPLACEMENTS TO RECEIVE TOPSOIL, SEED, FERTILIZER, AND EMAT.
 CL ELEVATIONS ARE GIVEN FOR INFORMATIONAL PURPOSES ONLY.
 CURB AND GUTTER GRADES ARE SHOWN TO THE FLANGE LINE IN 50' INCREMENTS. VERIFY GRADES MATCH EXISTING CONDITIONS AND PROPOSED CURB AND GUTTER DRAINS APPROPRIATELY.

BENCH MARKS			
NO.	STATION	ELEV.	DESCRIPTION
CP 5	135+66.4 RT	1153.70	MAG NAIL IN SIDEWALK



PROJECT NO: 6999-16-73	ROADWAY: MAIN STREET	COUNTY: MARATHON	PLAN	SHEET	E
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LEGEND:

- XXXXXX SAWCUT
- HMA PAVEMENT

CL CURVE DATA
CURVE 3

PI	145+52.462
N	142999.205
E	260551.189
DELTA	41°39'07"
D	12°03'44"
T	180.682'
L	345.307'
R	475.000'
LC	337.754'
LCB	S69°04'54"E
NORMAL CROWN	

PLAN NOTES:
SAWCUT REQUIRED AT ALL SPOT CURB AND GUTTER REPLACEMENTS AND SPOT SIDEWALK REPAIRS.

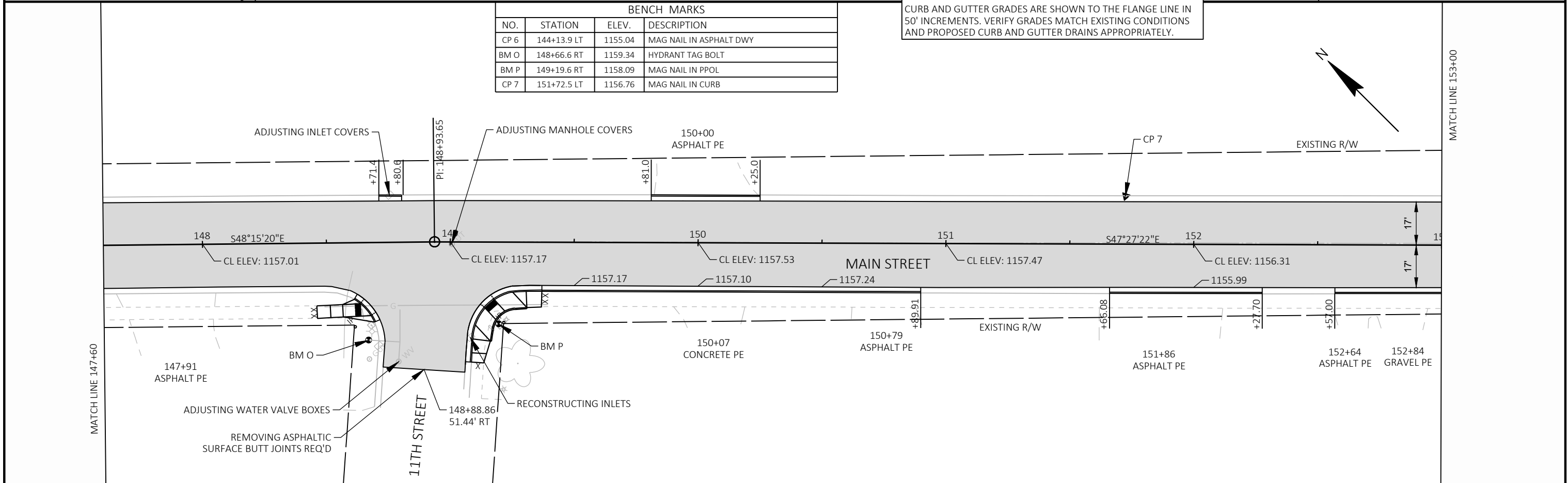
ALL AREAS DISTURBED BY CURB RAMP REPLACEMENTS TO RECEIVE TOPSOIL, SEED, FERTILIZER, AND EMAT.

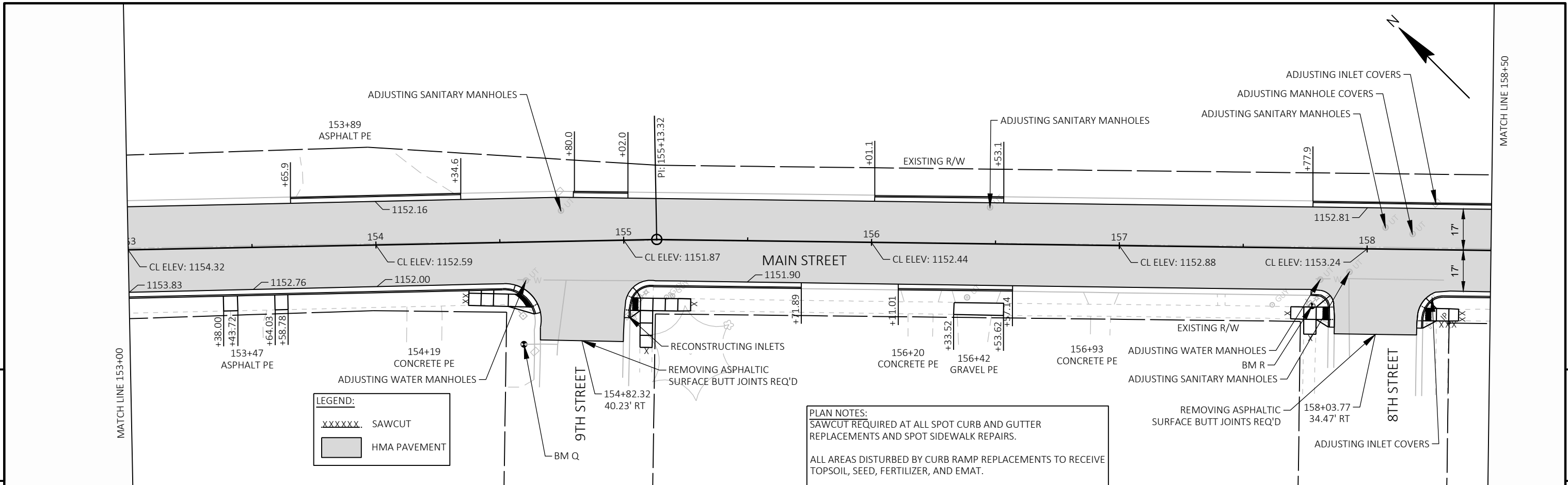
CL ELEVATIONS ARE GIVEN FOR INFORMATIONAL PURPOSES ONLY.

CURB AND GUTTER GRADES ARE SHOWN TO THE FLANGE LINE IN 50' INCREMENTS. VERIFY GRADES MATCH EXISTING CONDITIONS AND PROPOSED CURB AND GUTTER DRAINS APPROPRIATELY.

BENCH MARKS

NO.	STATION	ELEV.	DESCRIPTION
CP 6	144+13.9 LT	1155.04	MAG NAIL IN ASPHALT DWY
BM O	148+66.6 RT	1159.34	HYDRANT TAG BOLT
BM P	149+19.6 RT	1158.09	MAG NAIL IN PPOL
CP 7	151+72.5 LT	1156.76	MAG NAIL IN CURB





5

5

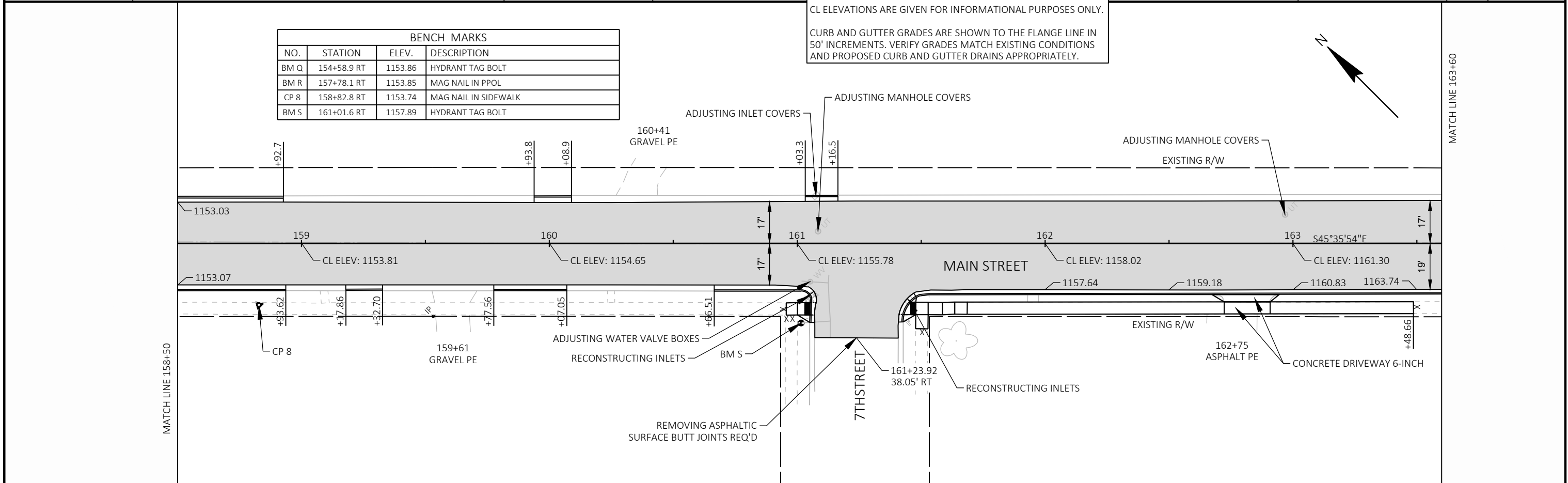
LEGEND:

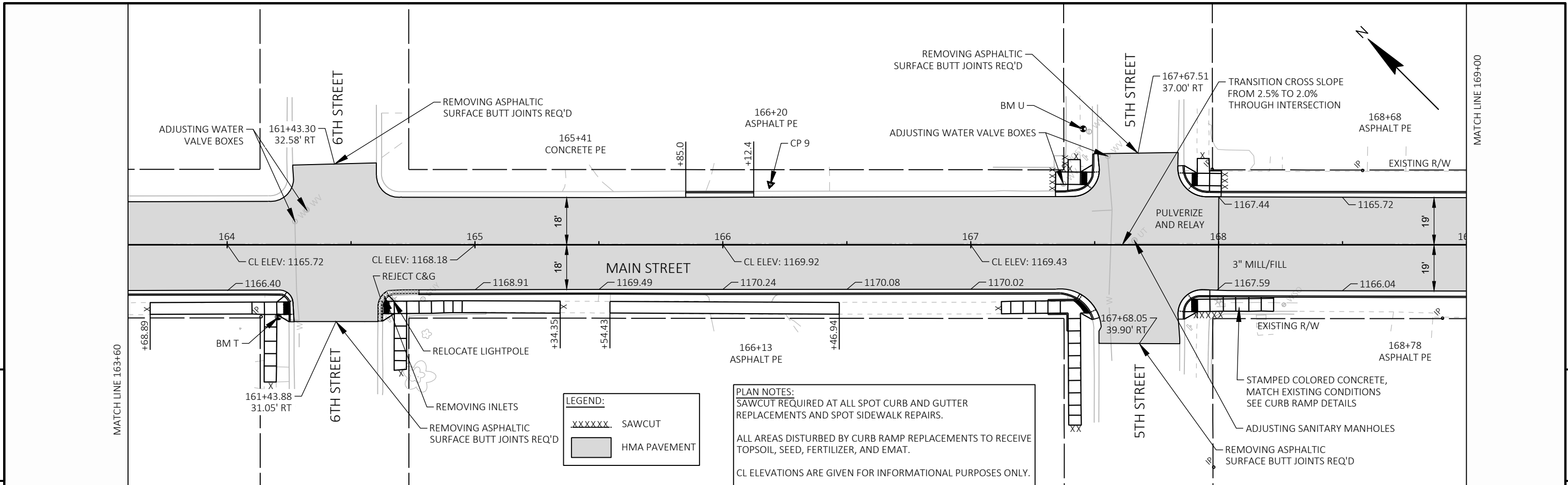
XXXXXX	SAWCUT
[Hatched Box]	HMA PAVEMENT

PLAN NOTES:
 SAWCUT REQUIRED AT ALL SPOT CURB AND GUTTER REPLACEMENTS AND SPOT SIDEWALK REPAIRS.
 ALL AREAS DISTURBED BY CURB RAMP REPLACEMENTS TO RECEIVE TOPSOIL, SEED, FERTILIZER, AND EMAT.
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 CURB AND GUTTER GRADES ARE SHOWN TO THE FLANGE LINE IN 50' INCREMENTS. VERIFY GRADES MATCH EXISTING CONDITIONS AND PROPOSED CURB AND GUTTER DRAINS APPROPRIATELY.

BENCH MARKS

NO.	STATION	ELEV.	DESCRIPTION
BM Q	154+58.9 RT	1153.86	HYDRANT TAG BOLT
BM R	157+78.1 RT	1153.85	MAG NAIL IN PPOL
CP 8	158+82.8 RT	1153.74	MAG NAIL IN SIDEWALK
BM S	161+01.6 RT	1157.89	HYDRANT TAG BOLT





5

5

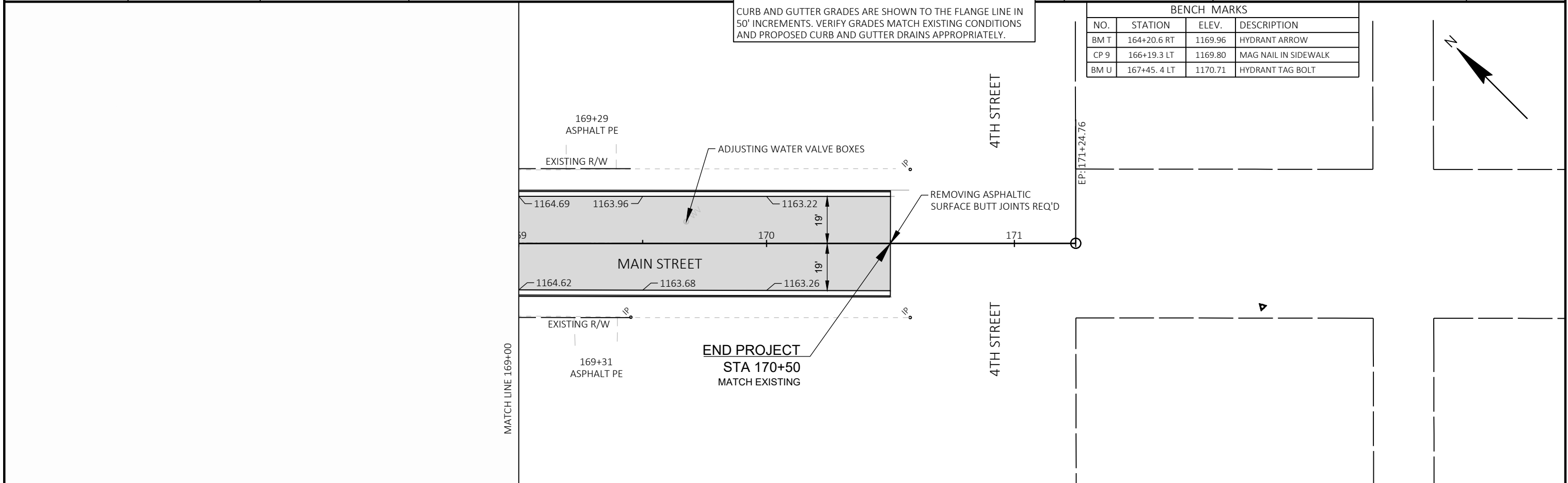
LEGEND:

XXXXXX	SAWCUT
[Shaded Box]	HMA PAVEMENT

PLAN NOTES:
 SAWCUT REQUIRED AT ALL SPOT CURB AND GUTTER REPLACEMENTS AND SPOT SIDEWALK REPAIRS.
 ALL AREAS DISTURBED BY CURB RAMP REPLACEMENTS TO RECEIVE TOPSOIL, SEED, FERTILIZER, AND EMAT.
 CL ELEVATIONS ARE GIVEN FOR INFORMATIONAL PURPOSES ONLY.
 CURB AND GUTTER GRADES ARE SHOWN TO THE FLANGE LINE IN 50' INCREMENTS. VERIFY GRADES MATCH EXISTING CONDITIONS AND PROPOSED CURB AND GUTTER DRAINS APPROPRIATELY.

BENCH MARKS

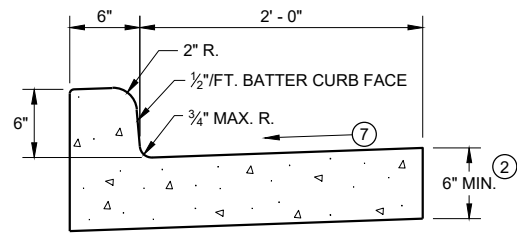
NO.	STATION	ELEV.	DESCRIPTION
BM T	164+20.6 RT	1169.96	HYDRANT ARROW
CP 9	166+19.3 LT	1169.80	MAG NAIL IN SIDEWALK
BM U	167+45.4 LT	1170.71	HYDRANT TAG BOLT



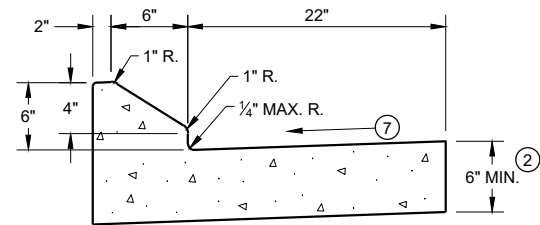
PROJECT NO: 6999-16-73	ROADWAY: MAIN STREET	COUNTY: MARATHON	PLAN	SHEET	E
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Standard Detail Drawing List

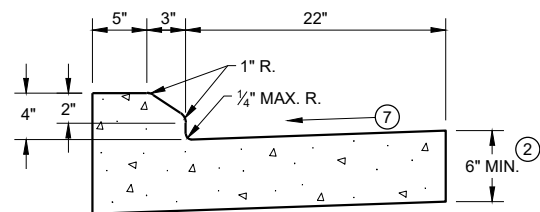
08D01-23A	CONCRETE CURB & GUTTER
08D01-23B	CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS
08D05-21A	CURB RAMPS TYPES 1 AND 1-A
08D05-21B	CURB RAMPS TYPES 2 AND 3
08D05-21C	CURB RAMPS TYPES 4A AND 4A1
08D05-21D	CURB RAMPS TYPE 4B AND 4B1
08D05-21F	CURB RAMPS RADIAL DETECTABLE WARNING FIELD APPLICATIONS
08D05-21G	CURB RAMPS RECTANGULAR AND RADIAL DETECTABLE WARNING PLATES
08D20-01	DRIVEWAYS WITH CURB & GUTTER RETURNS
08E10-02	INLET PROTECTION TYPE A, B, C AND D
13C19-03	HMA LONGITUDINAL JOINTS
15C02-09A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-09C	DETOUR SIGNING FOR MAINLINE CLOSURES
15C07-15C	PAVEMENT MARKING ARROWS
15C08-23A	PERMANENT LONGITUDINAL PAVEMENT MARKINGS
15C08-23C	PAVEMENT MARKING (TURN LANES)
15C08-23D	PAVEMENT MARKING (TURN LANES)
15C11-10B	CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS
15C18-08A	MEDIAN ISLAND MARKING PAVEMENT MARKINGS
15C33-04	STOP LINE AND CROSSWALK PAVEMENT MARKING
15D30-09A	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09B	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09C	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09D	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09E	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09F	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09G	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09H	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09I	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09J	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09K	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09L	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION



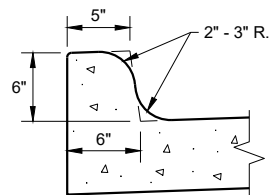
TYPES A¹ & D



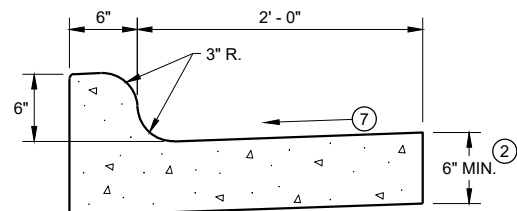
6" SLOPED CURB TYPES G¹ & J



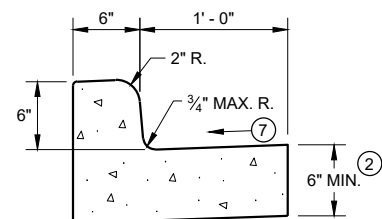
4" SLOPED CURB TYPES G¹ & J



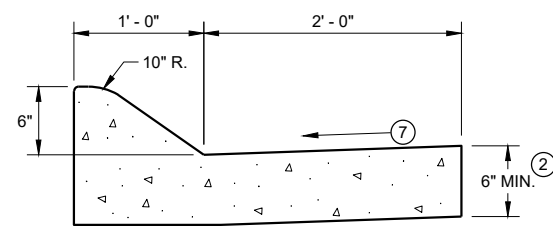
TYPES K¹ & L
(OPTIONAL CURB SHAPE)



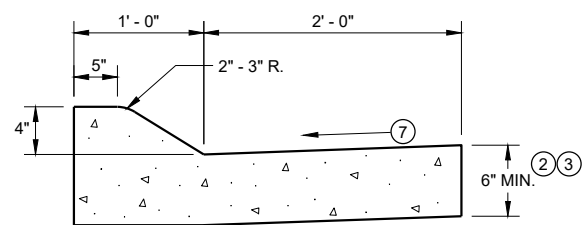
TYPES K¹ & L
CONCRETE CURB AND GUTTER 30"



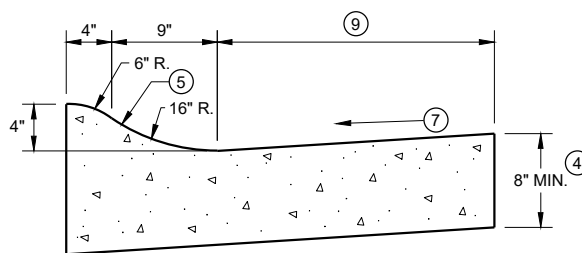
TYPES A¹ & D
CONCRETE CURB AND GUTTER 18"



6" SLOPED CURB TYPES A¹ & D

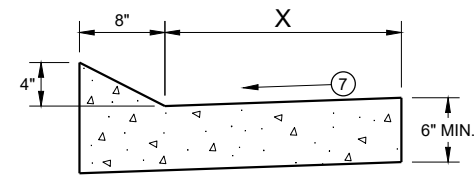


4" SLOPED CURB TYPES A¹ & D
CONCRETE CURB AND GUTTER 36"



4" SLOPED CURB TYPES R¹ & T

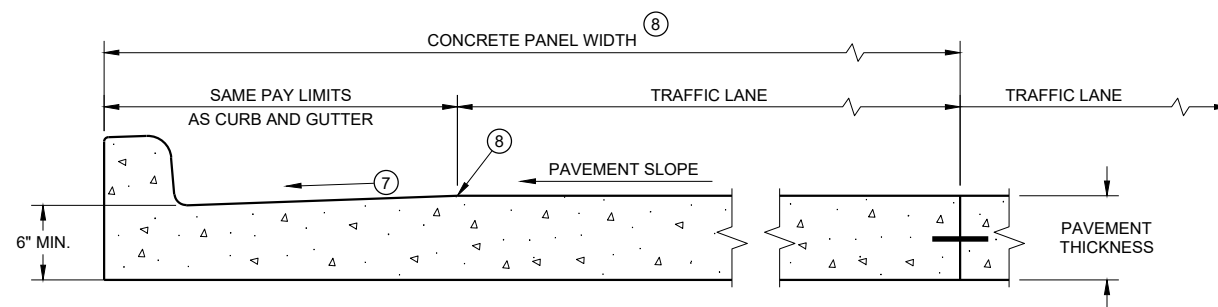
TBT & TBTT	X
30"	22"
36"	28"



TYPES TBT & TBTT¹
CONCRETE CURB AND GUTTER

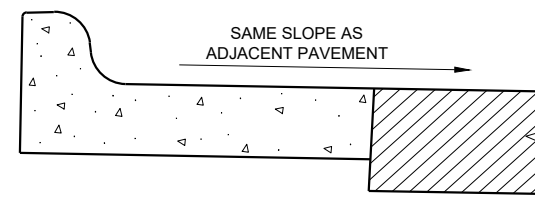
PAVEMENT THICKNESS AND MAXIMUM CONCRETE PANEL WIDTH TABLE

PAVEMENT THICKNESS	MAXIMUM PANEL WIDTH
LESS THAN 10"	12'
10" & ABOVE	15'



PARTIAL SECTION OF PAVEMENT* WITH INTEGRAL CURB AND GUTTER

* BIKE LANE IS NOT SHOWN



REVERSE SLOPE GUTTER⁶
(TYPICAL FOR ALL CURB & GUTTER TYPES)

GENERAL NOTES

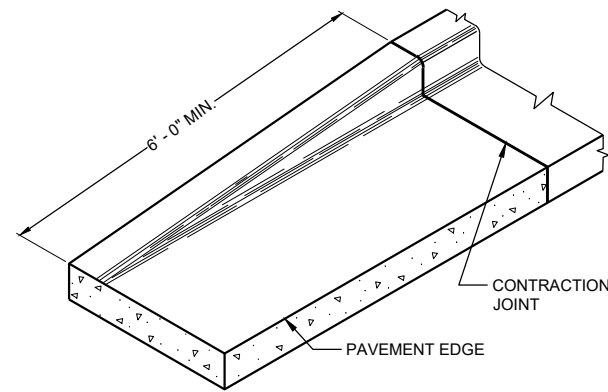
DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

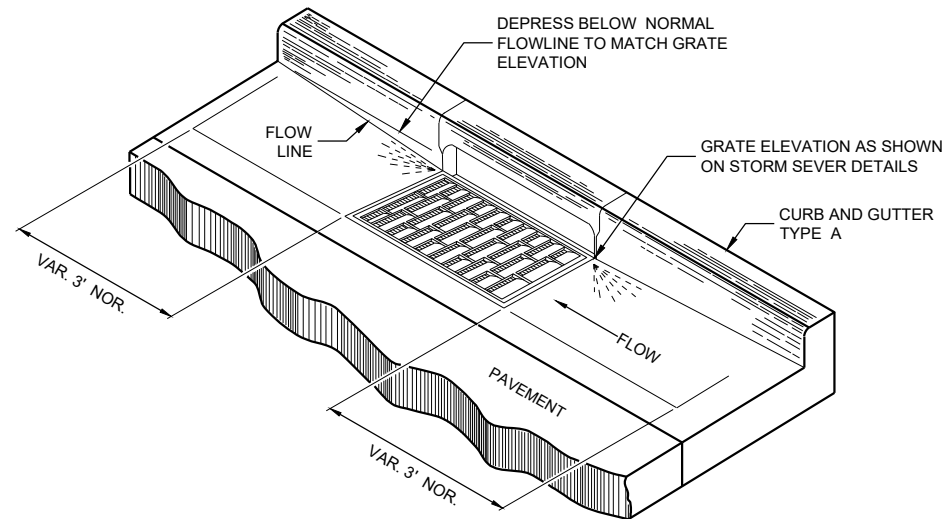
INTEGRAL CURB AND GUTTER SHALL CONFORM TO THE DETAILS SHOWN FOR CONCRETE CURB AND GUTTER INCLUDING THE TRANSVERSE GUTTER SLOPE.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ③ USE 8" MINIMUM GUTTER THICKNESS WHEN USED WITH AN ADJACENT CONCRETE TRUCK APRON PLACED BEHIND BACK OF CURB.
- ④ THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 8" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑤ UNLESS OTHERWISE NOTED, FOR STAKING PURPOSES THE FACE OF CURB IS 6" FROM THE BACK OF CURB.
- ⑥ WHEN REVERSE SLOPE GUTTER IS REQUIRED, THE LOCATION(S) WILL BE SHOWN ELSEWHERE IN THE PLAN.
- ⑦ USE 4% GUTTER CROSS SLOPE UNLESS OTHERWISE NOTED IN THE PLANS.
- ⑧ INCLUDE LONGITUDINAL JOINT AND TIE BARS ALONG LANE EDGE WHEN CONCRETE PANEL WIDTH EXCEEDS THE MAXIMUM WIDTH PER TABLE BELOW. LONGITUDINAL JOINT(S) ARE NOT ALLOWED WITHIN TRAFFIC LANES AND BIKE LANES. LONGITUDINAL JOINT MAY BE SAWED.
- ⑨ CONCRETE CURB AND GUTTER 4-INCH SLOPED 30-INCH TYPE "R" AND "T" = 17 INCHES
CONCRETE CURB AND GUTTER 4-INCH SLOPED 36-INCH TYPE "R" AND "T" = 23 INCHES



END SECTION CURB AND GUTTER



DETAIL OF CURB AND GUTTER AT INLETS

(TYPICAL H INLET COVER SHOWN)

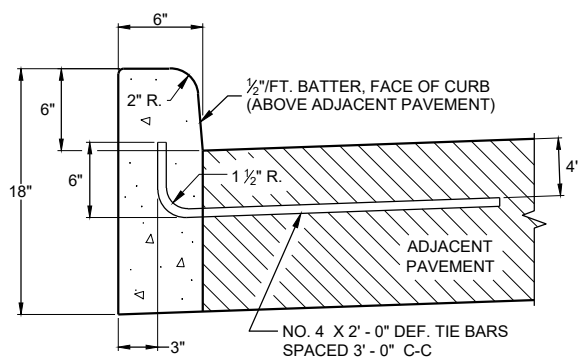
GENERAL NOTES

DETAILS OF CONSTRUCTION AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE CONTRACT.

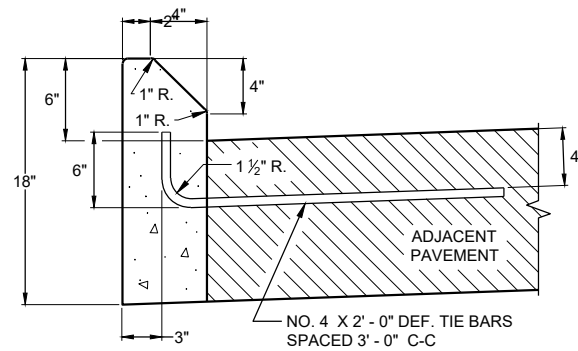
PAVEMENT TIES AND TIE BARS SHALL BE EPOXY COATED IN CONFORMANCE WITH SUBSECTION 505.2.6.2 OF THE STANDARD SPECIFICATIONS.

UNLESS OTHERWISE SHOWN ON THE TYPICAL CROSS SECTIONS, THE BASE AGGREGATE AND COMMON EXCAVATION LIMITS ARE 2' - 0" BEHIND THE BACK OF CURBS.

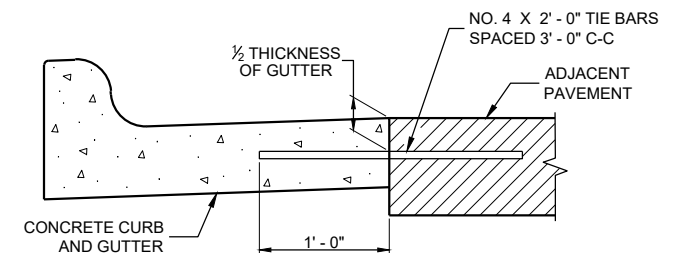
- ① TIE BARS ARE REQUIRED FOR CURB AND GUTTERS TYPES A, G, K, R, AND TBTT.
- ② THE BOTTOM OF CURB AND GUTTER MAY BE CONSTRUCTED EITHER LEVEL OR PARALLEL TO THE SLOPE OF THE SUBGRADE OR BASE AGGREGATE PROVIDED A 6" MINIMUM GUTTER THICKNESS IS MAINTAINED.
- ⑩ REFER TO SDD 08D18 AND 08D19 FOR ADDITIONAL DRIVEWAY ENTRANCE CURB DETAILS.
- ⑪ PLACE 1" THICK EXPANSION JOINT MATERIAL BETWEEN VERTICAL FACE CURB TYPES EXTENDING FROM THE TOP OF CURB TO 1 INCH BELOW THE ADJOINING CONCRETE SURFACE. RIGID CONCRETE STRUCTURES INCLUDE RAISED CONCRETE MEDIANS, CONCRETE SAFETY ISLANDS, SPLITTER ISLANDS, OR LOCATIONS IDENTIFIED ON THE PLANS.



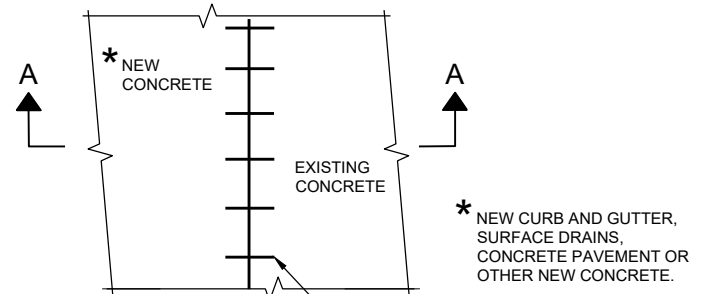
TYPES A^① & D



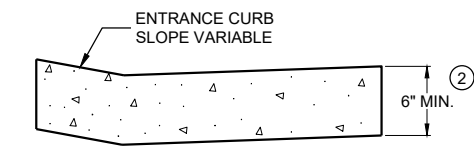
**TYPES G^① & J
CONCRETE CURB**



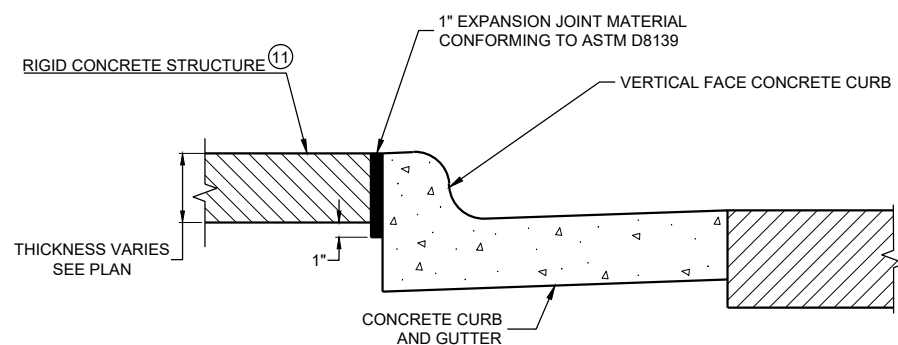
TYPICAL TIE BAR LOCATION^①



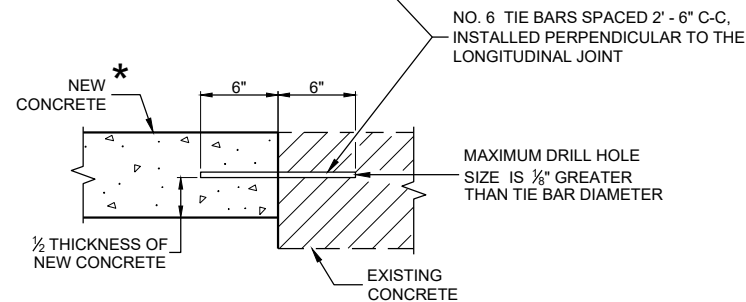
PLAN VIEW



**DRIVEWAY ENTRANCE CURB^⑩
(WHEN DIRECTED BY THE ENGINEER)**



EXPANSION JOINT DETAIL FOR VERTICAL CURB ABUTTING A RIGID STRUCTURE^⑪



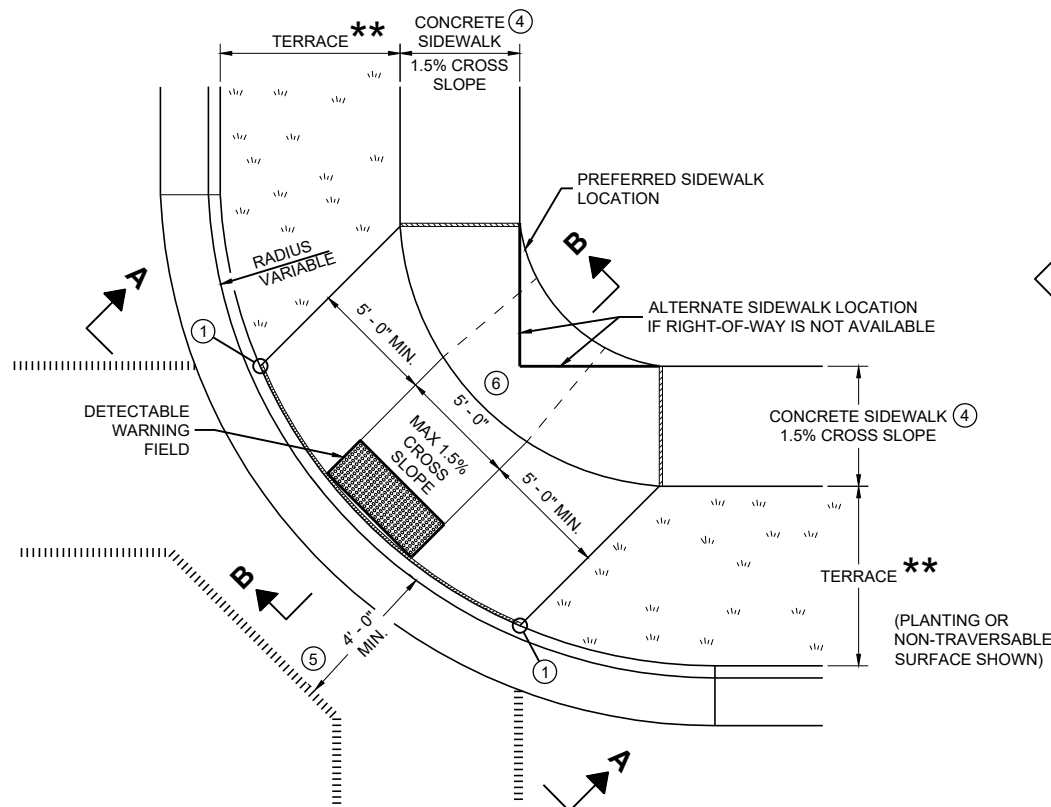
**SECTION A - A
TIE BARS DRILLED INTO EXISTING PAVEMENT**

CONCRETE CURB, TIES AND CURB AND GUTTER APPLICATIONS

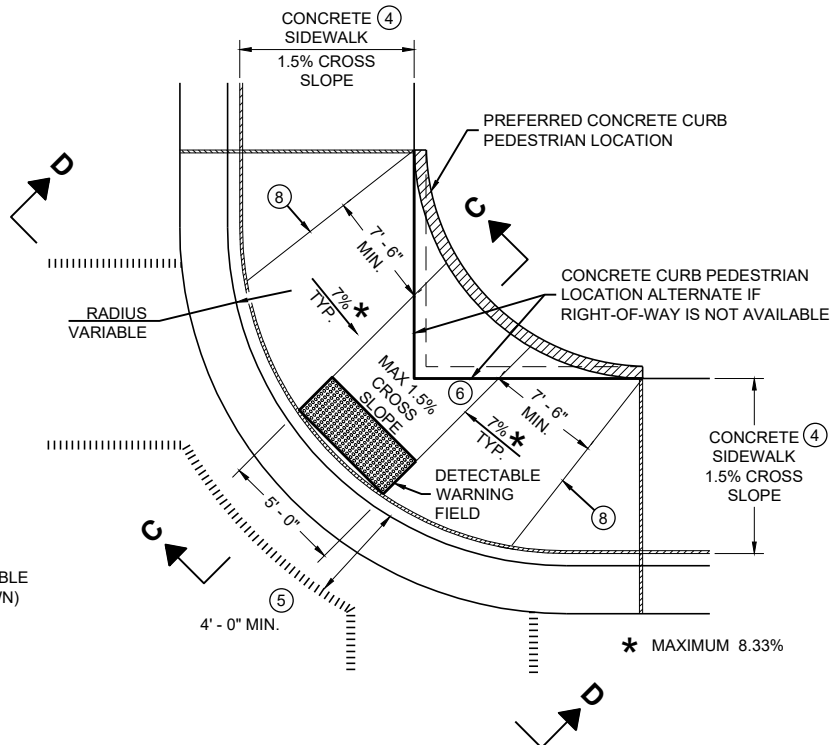
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE May 2023 /S/ Rodney Taylor
ROADWAY STANDARDS DEVELOPMENT ENGINEER

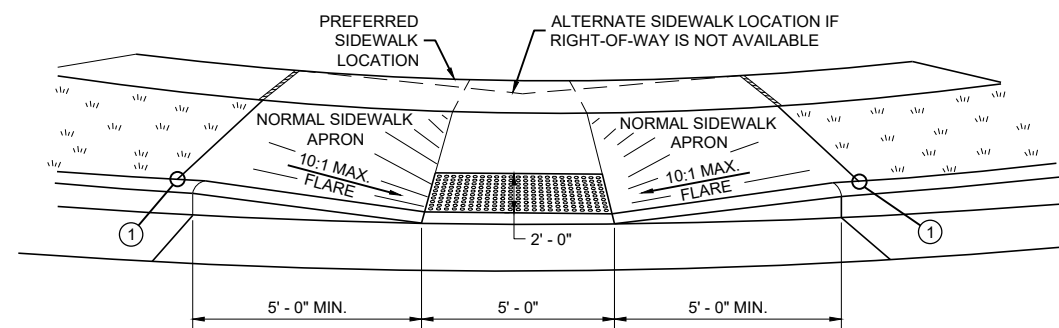
FHWA



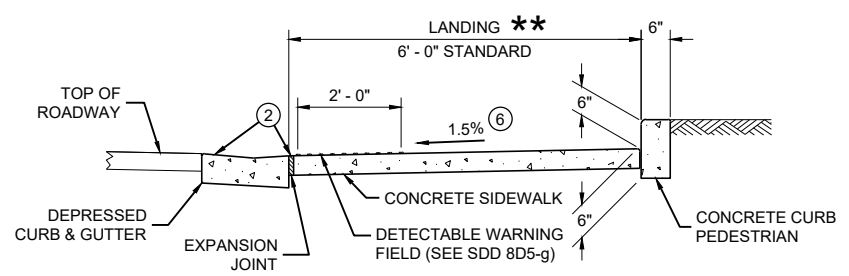
**PLAN VIEW
CURB RAMP TYPE 1
(CENTER OF CORNER RADIUS)**



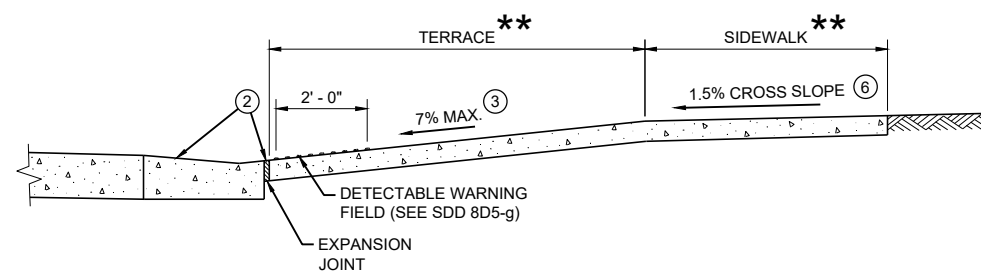
**PLAN VIEW
CURB RAMP TYPE 1 - A
(NO TERRACE)**



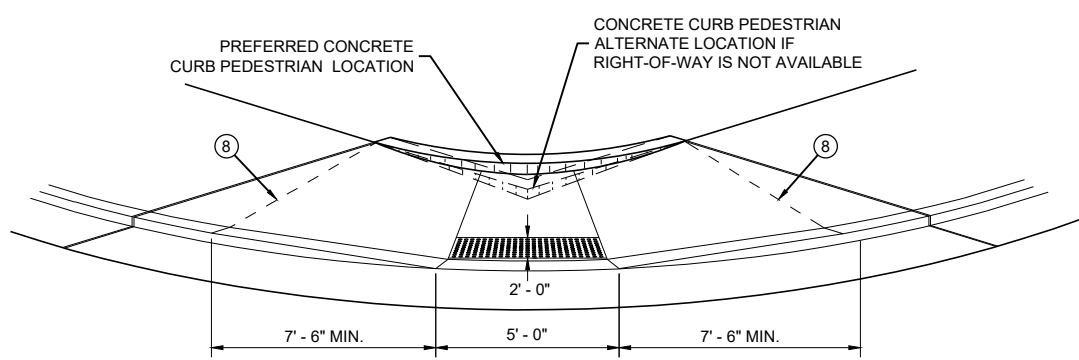
VIEW A - A FOR TYPE 1



SECTION C - C FOR TYPE 1 - A



SECTION B - B FOR TYPE 1



VIEW D - D FOR TYPE 1 - A

GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- WHEN NECESSARY, THE SIDEWALK ELEVATION MAY BE LOWERED TO MEET THE HIGH POINT ON THE RAMP.
- TYPE 1 CURB RAMPS SHALL HAVE A NORMAL SIDEWALK APRON AND CURB ON BOTH SIDES OF RAMP.
- DETECTABLE WARNING FIELD SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS "CURB RAMP DETECTABLE WARNING FIELD". THE CONCRETE PEDESTRIAN CURB, IF NEEDED, SHALL BE MEASURED AND PAID BY THE LINEAR FOOT AS "CONCRETE CURB PEDESTRIAN". CONCRETE SIDEWALK IN THE CURB RAMP AREA SHALL BE MEASURED AND PAID BY THE SQUARE FOOT AS CONCRETE SIDEWALK, INCLUDING THE AREA UNDER THE DETECTABLE WARNING FIELD.
- SELECT CURB RAMP DETECTABLE WARNING FIELD MATERIALS AND DEVICES FROM THE DEPARTMENT'S APPROVED MATERIALS LIST. THE COLOR OF THE DETECTABLE WARNING FIELD IS SPECIFIED ELSEWHERE AND IS INCIDENTAL TO THE BID ITEM OF "CURB RAMP DETECTABLE WARNING FIELD"
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- SURFACE TEXTURE OF THE RAMP SHALL BE OBTAINED BY COARSE BROOMING TRANSVERSE TO THE SLOPE OF THE RAMP.
- ① THIS POINT IS AN EXTENSION OF OUTSIDE EDGE OF APPROACHING SIDEWALK WHERE IT MEETS THE BACK OF CONCRETE CURB. POINT LOCATION MAY BE ADJUSTED TO ALIGN WITH BEGINNING OF FULL-HEIGHT CURB IF THIS DISTANCE IS SHORT.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ MAXIMUM 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑤ PROVIDE A LEVEL LANDING IN THE STREET AND GUTTER AREA (2% MAXIMUM SLOPE IN ANY DIRECTION). WHEN THE GUTTER SLOPE EXCEEDS 2%, CONSTRUCT THE LEVEL LANDING IN THE STREET AREA. 4 FOOT WIDTH IS MEASURED FROM THE FLANGE LINE
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.

LEGEND

- 1/2" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT FIELD LOCATED
- PAVEMENT MARKING CROSSWALK (WHITE)

**CURB RAMPS
TYPE 1 AND 1-A**

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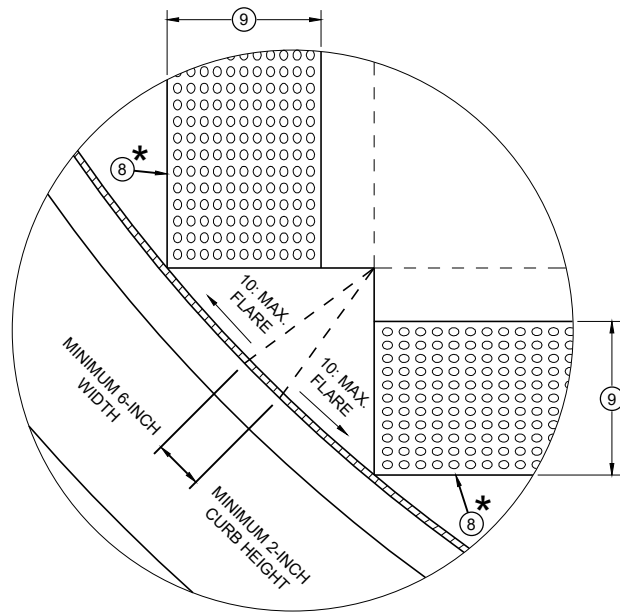
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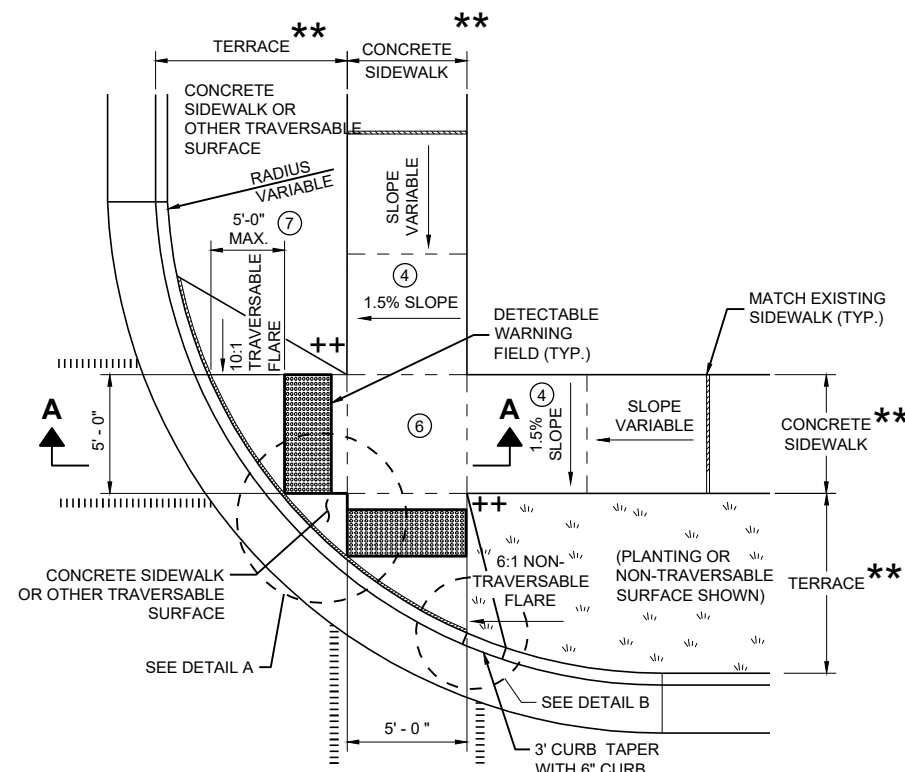
SDD 08D05-21a

SDD 08D05-21a

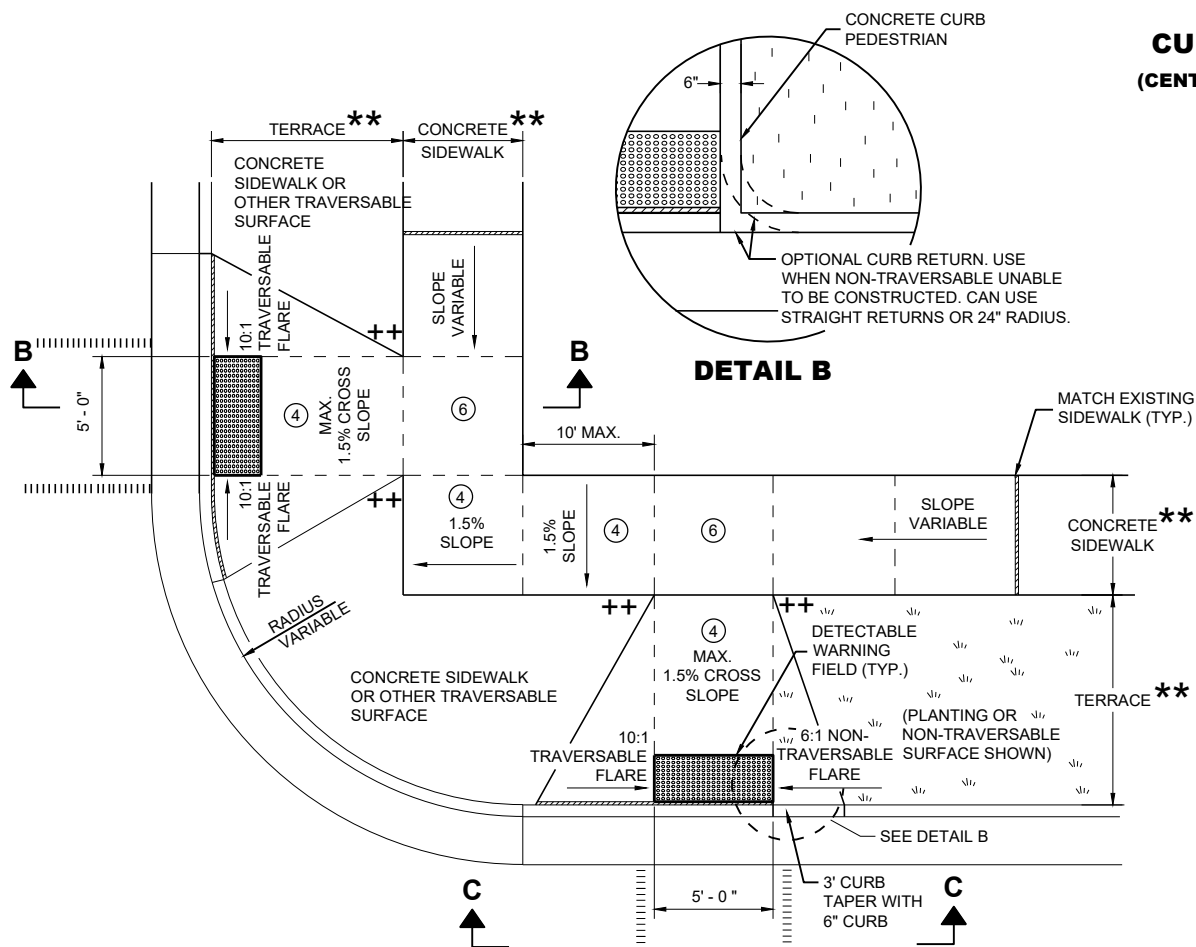
** WIDTH SHOWN ELSEWHERE IN THE PLANS



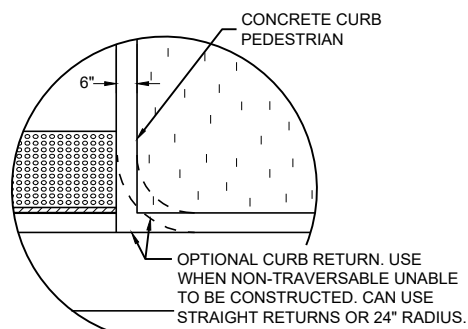
DETAIL A



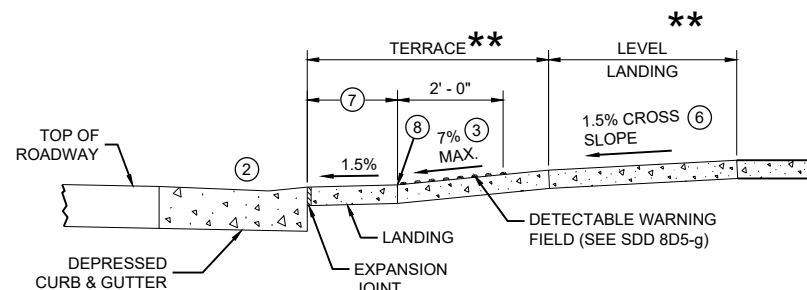
**PLAN VIEW
CURB RAMP TYPE 2
(CENTER OF CORNER RADIUS)**



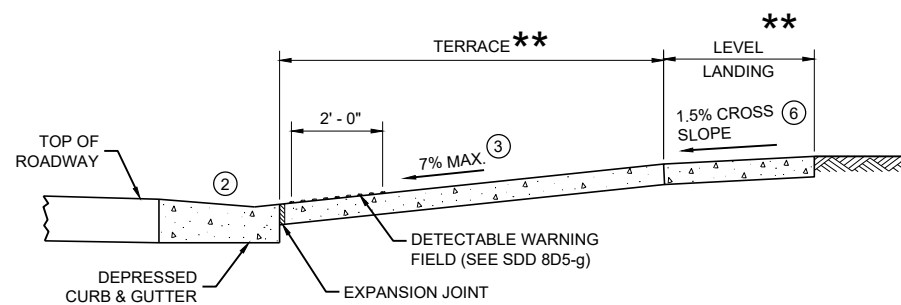
**PLAN VIEW
CURB RAMP TYPE 3
(OUTSIDE OF CROSSWALK AREA)**



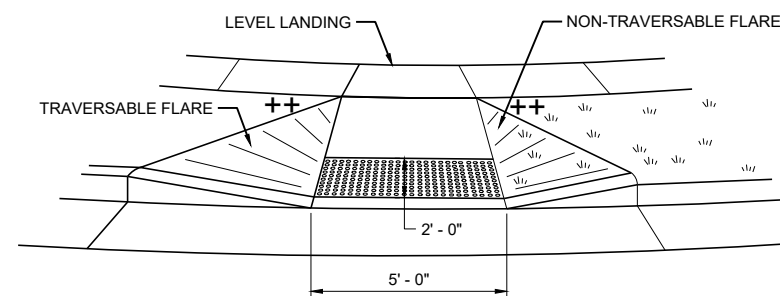
DETAIL B



SECTION A - A FOR TYPE 2



SECTION B - B FOR TYPE 3



VIEW C - C FOR TYPE 3

GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE (2.67% OR LESS) AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET X 5 FEET.
- ⑦ WHEN GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑨ WHEN DISTANCE IS LESS THAN 6' - 0", IT MAY BE DIFFICULT TO ACHIEVE A 7% SLOPE OR FLATTER ALONG THE RAMP. REDUCE CURB HEIGHT IN TRIANGLE AREA TO ACHIEVE 7% SLOPE OR FLATTER ON RAMP. CONSTRUCT 2-INCH MINIMUM CURB HEIGHT BETWEEN 10:1 FLARES.

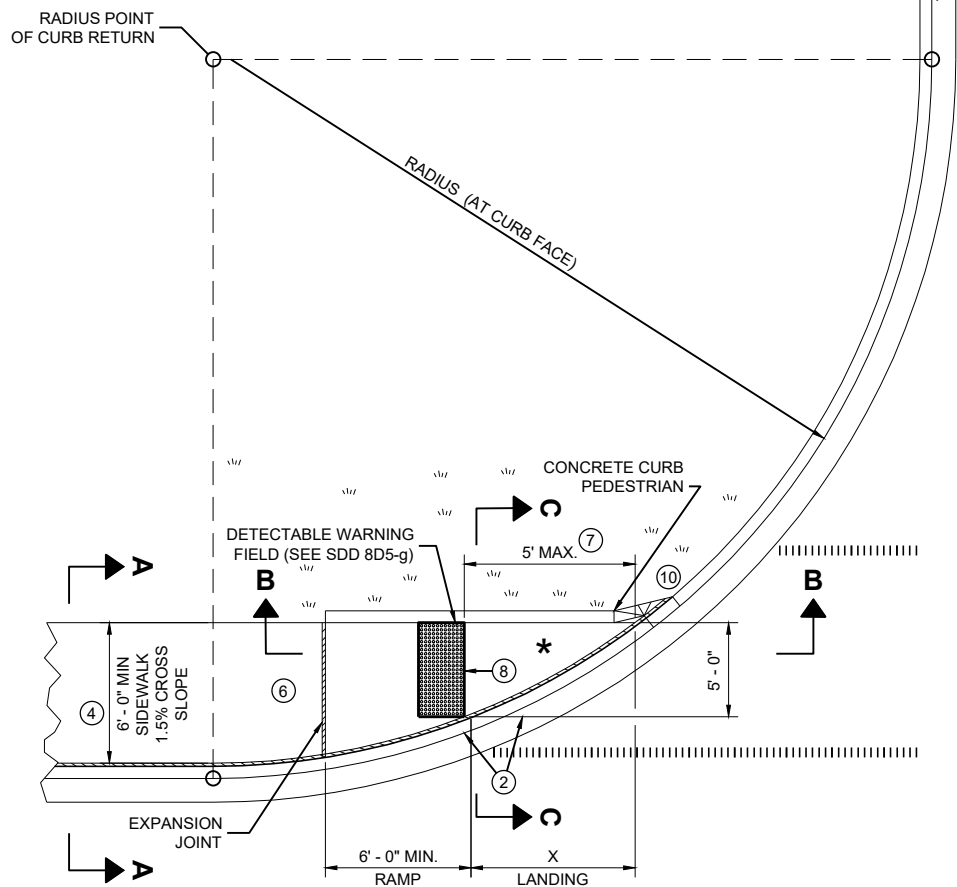
- * MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK
- ** WIDTH SHOWN ELSEWHERE IN THE PLANS
- ++ CONSTRUCT 6" WEDGE TO AVOID CONCRETE BREAKAGE

LEGEND

- 1/2" EXPANSION JOINT SIDEWALK
- - - CONTRACTION JOINT SIDEWALK
- ||||| PAVEMENT MARKING CROSSWALK (WHITE)

**CURB RAMPS
TYPE 2 AND 3**

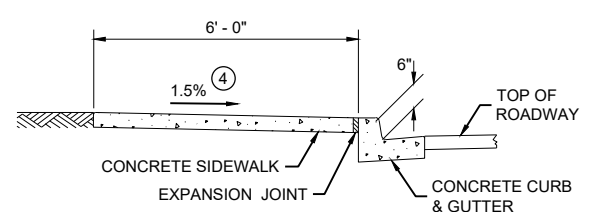
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**PLAN VIEW
CURB RAMP TYPE 4A**

RADIUS (AT CURB FACE)	X
10 FEET	4' - 7"

INTERMEDIATE RADII CAN BE INTERPOLATED



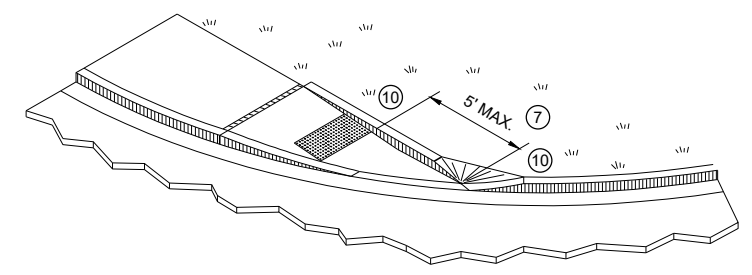
SECTION A - A FOR TYPE 4A

GENERAL NOTES

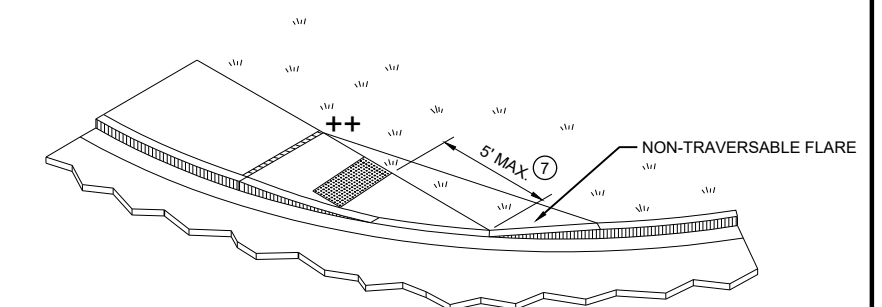
- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4 - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑦ WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑩ INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.

LEGEND

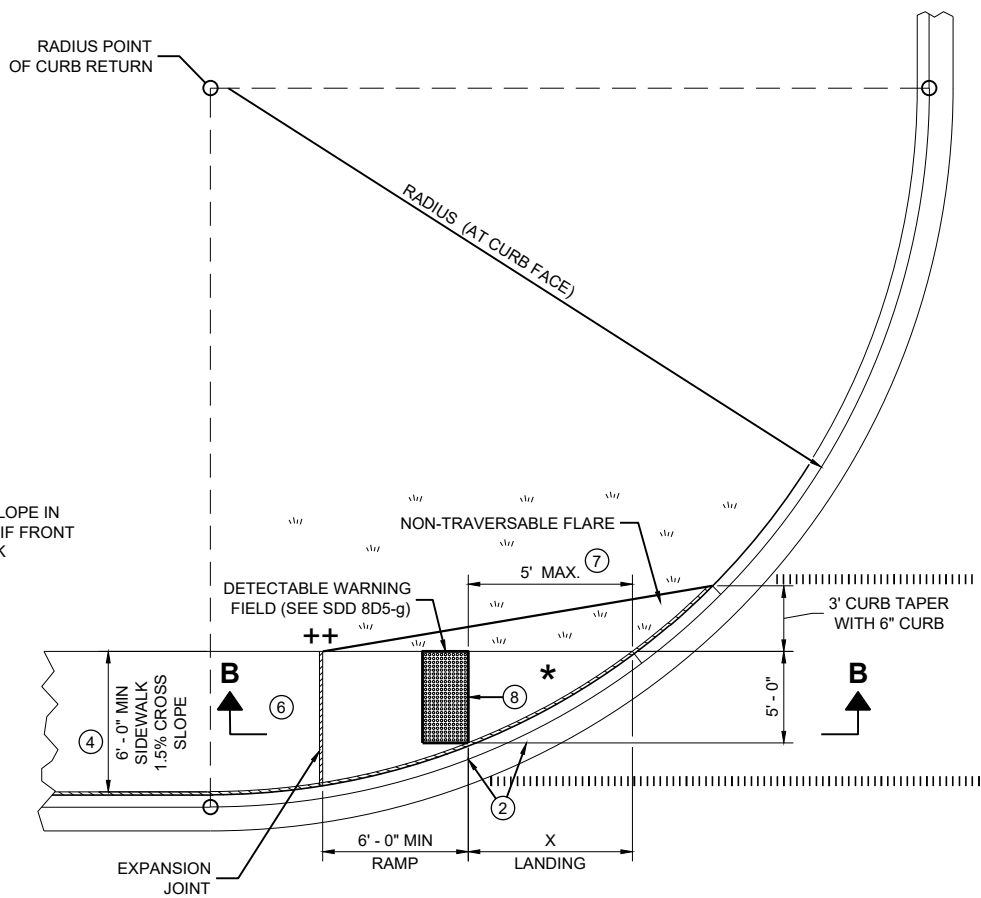
- 1/2" EXPANSION JOINT SIDEWALK
- - - - - CONTRACTION JOINT SIDEWALK
- ||||||| PAVEMENT MARKING CROSSWALK (WHITE)



ISOMETRIC VIEW FOR TYPE 4A



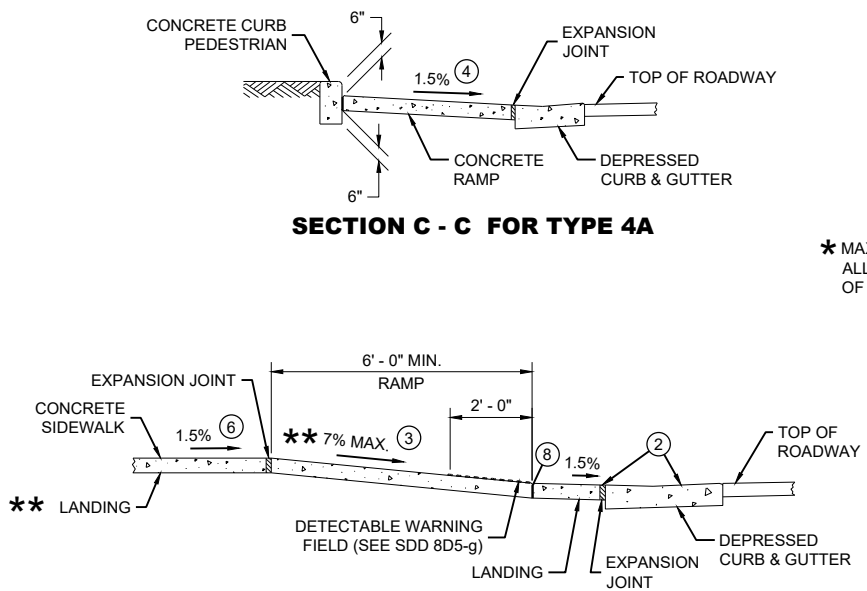
ISOMETRIC VIEW FOR TYPE 4A1



**PLAN VIEW
CURB RAMP TYPE 4A1**

* MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS IF FRONT OF GRADE BREAK

++ CONSTRUCT 6" WEDGE TO AVOID CONCRETE BREAKAGE

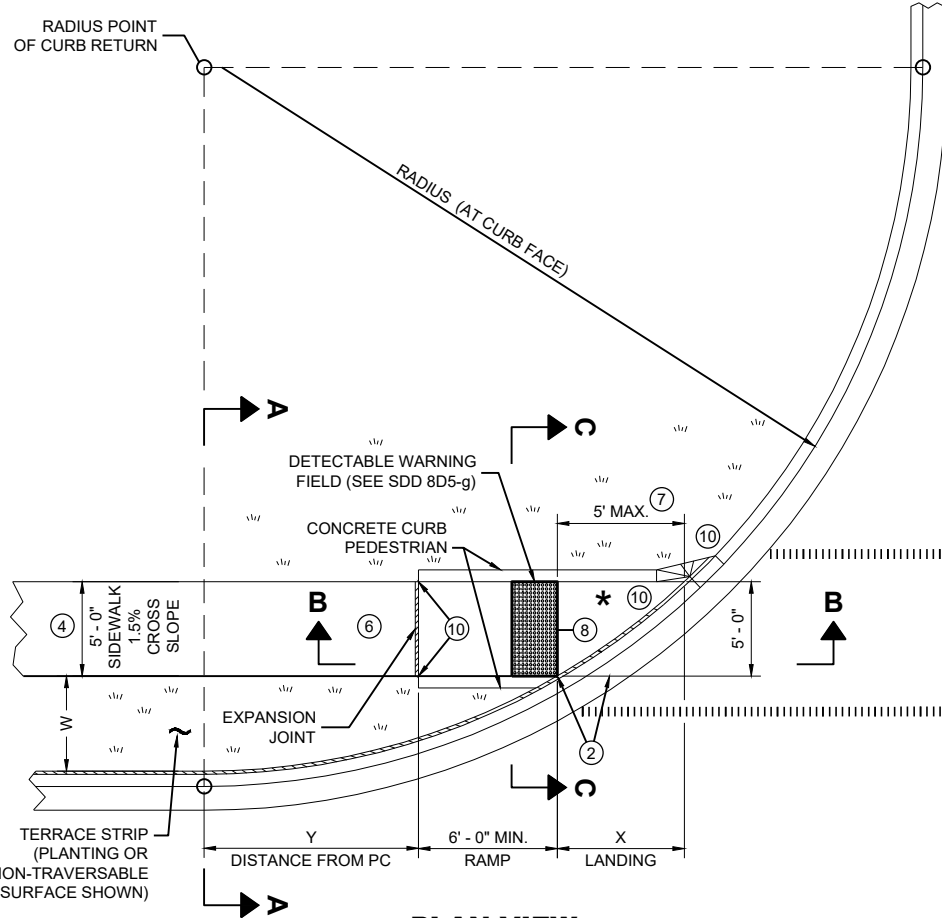


**SECTION B - B FOR
TYPE 4A AND TYPE 4A1**

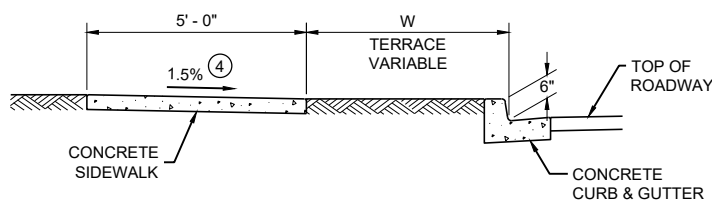
** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

**CURB RAMPS
TYPE 4A AND 4A1**

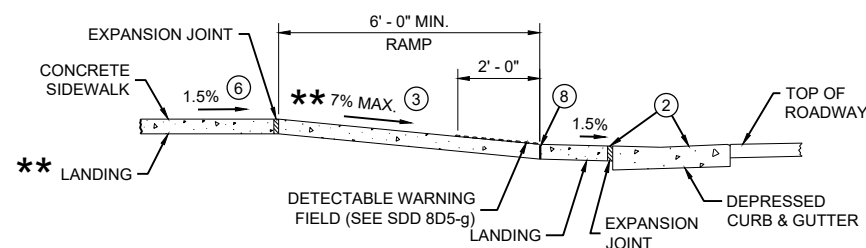
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PLAN VIEW CURB RAMP TYPE 4B



SECTION A - A FOR TYPE 4B

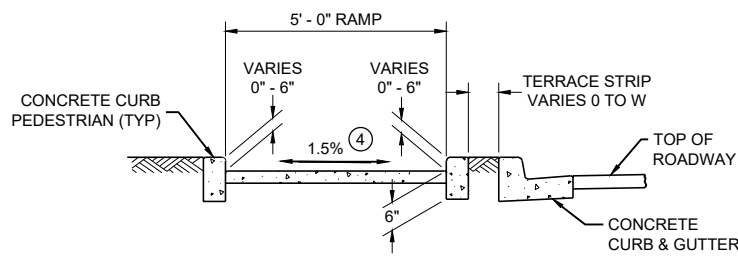


SECTION B - B FOR TYPE 4B AND TYPE 4B1

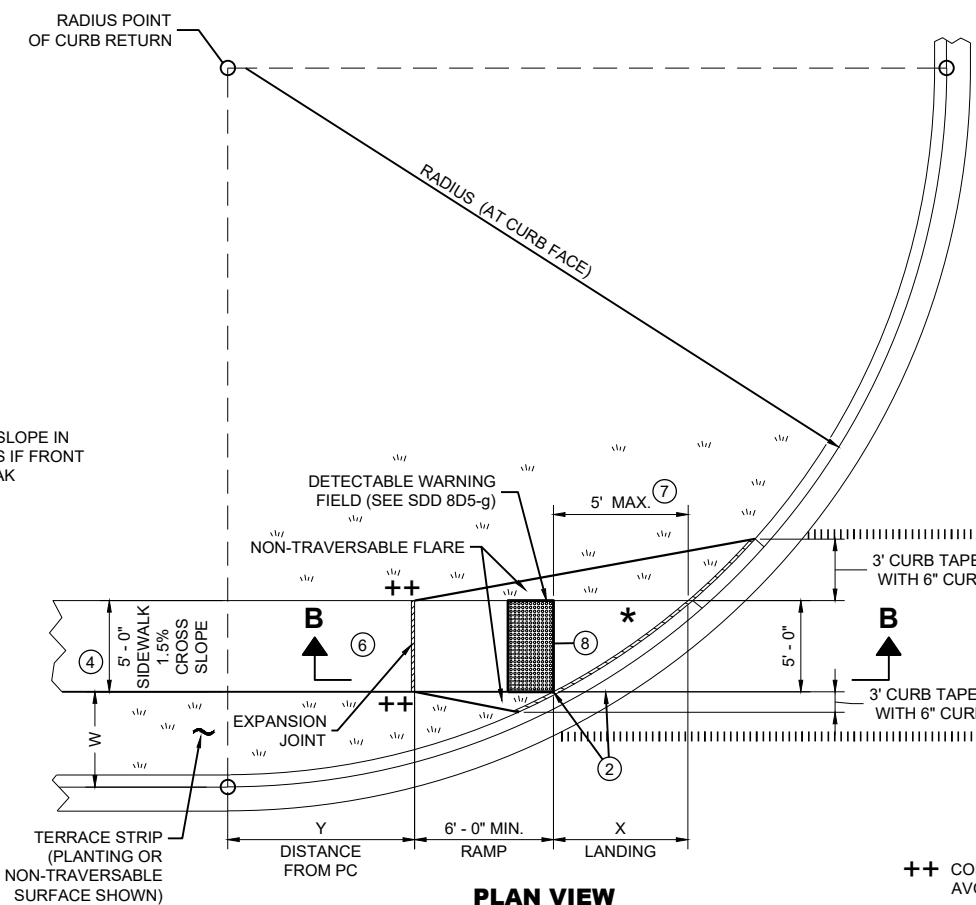
** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

RADIUS (AT CURB FACE)	W = 3' - 0"		W = 4' - 0"		W = 5' - 0"		W = 6' - 0"		W = 7' - 0"		W = 8' - 0"		W = 9' - 0"		W = 10' - 0"	
	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y	X	Y
10 FEET	2' - 10 1/4"	0' - 5"	2' - 1"	1' - 4 1/2"	1' - 5"	2' - 1"	0' - 10"	2' - 7 1/2"	0' - 3 1/4"	3' - 0 1/4"						
15 FEET	4' - 6 3/4"	2' - 1 3/4"	3' - 9"	3' - 5 3/4"	3' - 1 1/4"	4' - 6"	2' - 6 3/4"	5' - 4 1/2"	2' - 1"	6' - 1"	1' - 8"	6' - 8 1/2"	1' - 3 1/4"	7' - 2 1/2"	0' - 10 3/4"	7' - 7 1/4"
20 FEET			4' - 11 1/2"	5' - 1 3/4"	4' - 3 1/4"	6' - 5 1/2"	3' - 8 3/4"	7' - 7"	3' - 3"	8' - 6 1/2"	2' - 10"	9' - 4 1/2"	2' - 5 1/2"	10' - 1 1/4"	2' - 1 1/4"	10' - 9"
30 FEET									4' - 10 3/4"	12' - 5 3/4"	4' - 5 1/2"	13' - 7 3/4"	4' - 0 3/4"	14' - 8 1/2"	3' - 8 1/2"	15' - 8 1/4"
40 FEET															4' - 10 3/4"	19' - 8 1/4"

INTERMEDIATE RADII CAN BE INTERPOLATED
 DIMENSION "Y" IS CALCULATED BASED ON 6'-0" RAMP LENGTH
 DIMENSION "X" IS CALCULATED BASED ON 5'-0" SIDEWALK WIDTH



SECTION C - C FOR TYPE 4B



PLAN VIEW CURB RAMP TYPE 4B1

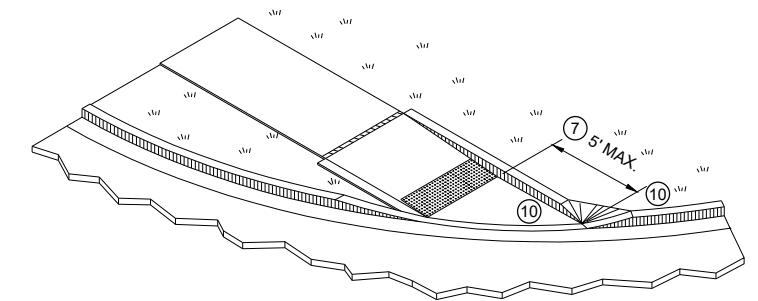
++ CONSTRUCT 6" WEDGE TO AVOID CONCRETE BREAKAGE

LEGEND

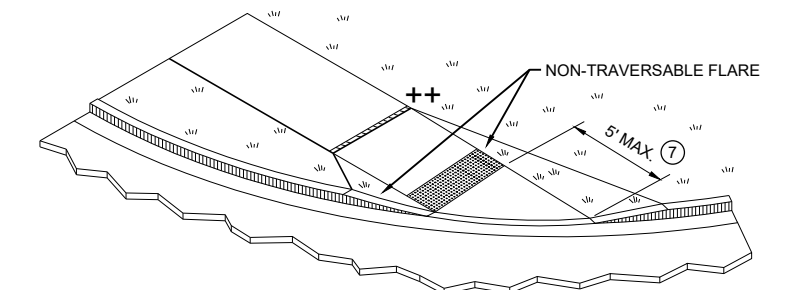
- ===== 1/2" EXPANSION JOINT SIDEWALK
- - - - - CONTRACTION JOINT SIDEWALK
- ||||| PAVEMENT MARKING CROSSWALK (WHITE)

GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- (2) GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4" - INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- (3) AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- (4) ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- (6) PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LEVEL LANDING SIZE IS 5 FEET BY 5 FEET.
- (7) WHEN THIS GRADE BREAK DISTANCE EXCEEDS 5 FEET, USE RADIAL DETECTABLE WARNING FIELD PER SDD 8D5-f.
- (8) PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- (10) INSTALL TRANSITION NOSE (INCIDENTAL TO OTHER PAY ITEMS). DO NOT MARK TRANSITION NOSE.



ISOMETRIC VIEW FOR TYPE 4B

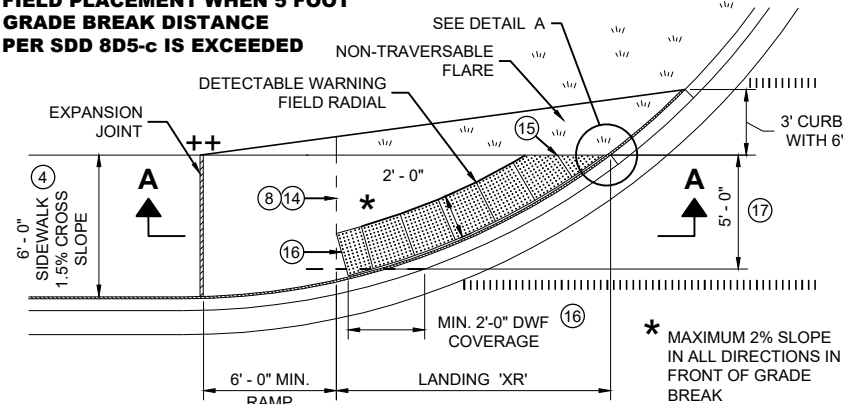


ISOMETRIC VIEW FOR TYPE 4B1

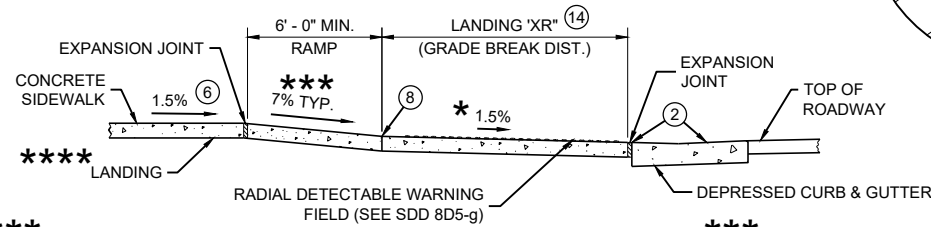
CURB RAMPS TYPE 4B AND 4B1

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**RADIAL DETECTABLE WARNING
FIELD PLACEMENT WHEN 5 FOOT
GRADE BREAK DISTANCE
PER SDD 8D5-c IS EXCEEDED**



**PLAN VIEW
CURB RAMP TYPE 4A1
(GRADE BREAK DISTANCE GREATER THAN 5 FEET)**



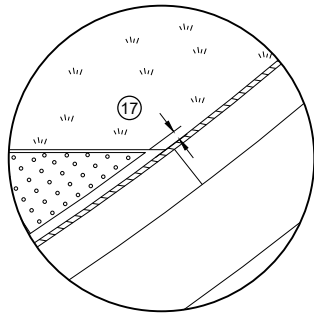
SECTION A - A FOR TYPE 4A1

**** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

*** MAXIMUM 8.33%

LEGEND

- ===== 1/2" EXPANSION JOINT SIDEWALK
- CONTRACTION JOINT SIDEWALK
- ||||||| PAVEMENT MARKING CROSSWALK (WHITE)

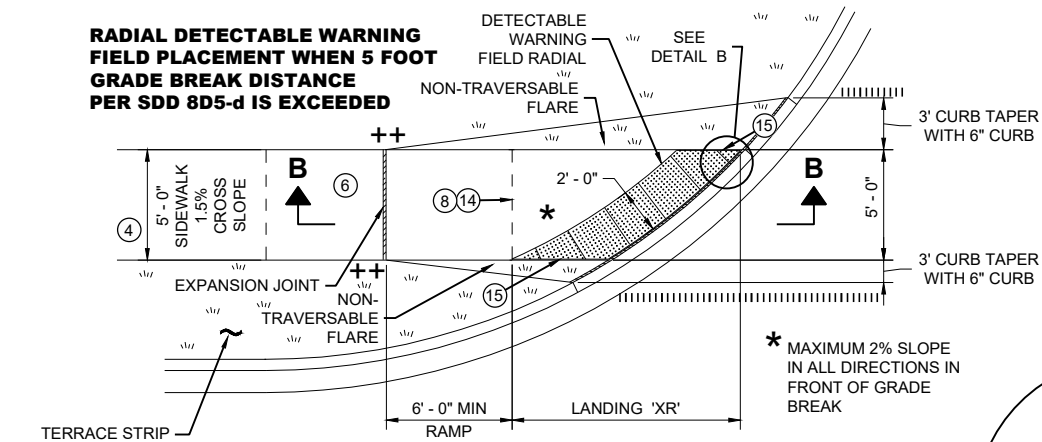


DETAIL A

GENERAL NOTES

- AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF RAMP ACCESS AREAS.
- DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.
- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AS A GROUP OR SIDE BY SIDE, SHALL BE FROM THE SAME MANUFACTURER.
- APPLY RADIAL DETECTABLE WARNING PLACEMENT SIMILARLY FOR TYPE 4A AND 4A1 CURB RAMPS AND SIMILARLY FOR TYPE 4B AND 4B1 CURB RAMPS. TYPE 4A AND 4B CURB RAMPS ARE NOT SHOWN.
- REFER TO SDD 8D5-g FOR ADDITIONAL RADIAL PLATE REQUIREMENTS.
- FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FILED ARE PROHIBITED.
- DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.
- ② GRADE CHANGE BETWEEN GUTTER FLAG SLOPE AND THE CURB RAMP SLOPE SHALL NOT EXCEED 11%. MAXIMUM GUTTER FLAG SLOPE IS 4%. PROVIDE LONGITUDINAL DRAINAGE AROUND CURB AND AWAY FROM CURB RAMP. NO VERTICAL LIPS OR DISCONTINUITIES GREATER THAN 1/4" INCH ARE ALLOWED. SLOPE OF CURB HEAD OPENING SHALL MATCH THE RAMP SLOPE, MINIMALLY 1.5% AND NOT TO EXCEED 7%. WHEN ADJACENT TO 1.5% LANDING, CONSTRUCT CURB HEAD OPENING AT 1.5% IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ③ AN 8.33% CURB RAMP SLOPE IS ALLOWABLE WITH FLATTENED GUTTER FLAG SLOPE AND NOT TO EXCEED 11% GRADE CHANGE.
- ④ ±0.5% CONSTRUCTION TOLERANCE IN SIDEWALK CROSS SLOPE. THE SIDEWALK CROSS SLOPE SHALL NOT EXCEED 2% WITHOUT PRIOR APPROVAL FROM THE ENGINEER.
- ⑥ PROVIDE A LEVEL LANDING (MAXIMUM 2% SLOPE) IN ANY DIRECTION OF PEDESTRIAN TRAVEL. STANDARD LANDING SIZE IS 5 FEET BY 5 FEET.
- ⑧ PROVIDE GRADE BREAK PERPENDICULAR TO DIRECTION OF WHEELCHAIR TRAVEL.
- ⑭ CONSULT ENGINEER IF GRADE BREAK LOCATION (END OF LANDING DIMENSION "XR") REQUIRES FIELD ADJUSTMENT WHEN ESTABLISHING FINAL RADIAL DETECTABLE WARNING FIELD LOCATION.
- ⑮ FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN 1/2" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.
- ⑯ USE 1' X 2" RECTANGULAR END PLATE AT END OF TYPE 4A1 RAMP AND PROVIDE MINIMUM 2' - 0" DETECTABLE WARNING FIELD COVERAGE (IN DIRECTION OF PEDESTRIAN TRAVEL) ALONG THE ENTIRE CURB RAMP WIDTH.
- ⑰ A MAXIMUM 3 INCH CONCRETE BORDER WIDTH IS ALLOWABLE IN FRONT OF RADIAL DETECTABLE WARNING FIELD FOR CONSTRUCTABILITY PURPOSES. CONCRETE BORDER WIDTH MAY VARY UP TO 1 INCH.

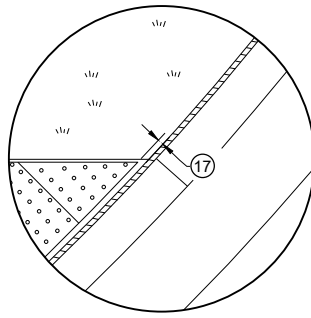
**RADIAL DETECTABLE WARNING
FIELD PLACEMENT WHEN 5 FOOT
GRADE BREAK DISTANCE
PER SDD 8D5-d IS EXCEEDED**



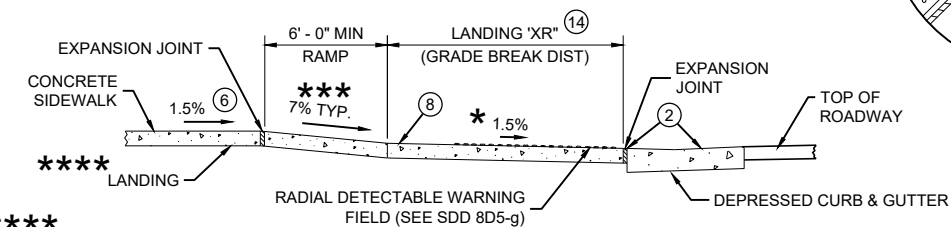
**PLAN VIEW
CURB RAMP TYPE 4B1
(GRADE BREAK DISTANCE GREATER THAN 5 FEET)**

TERRACE STRIP (PLANTING OR NON-TRAVERSABLE SURFACE SHOWN)

* MAXIMUM 2% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK



DETAIL B

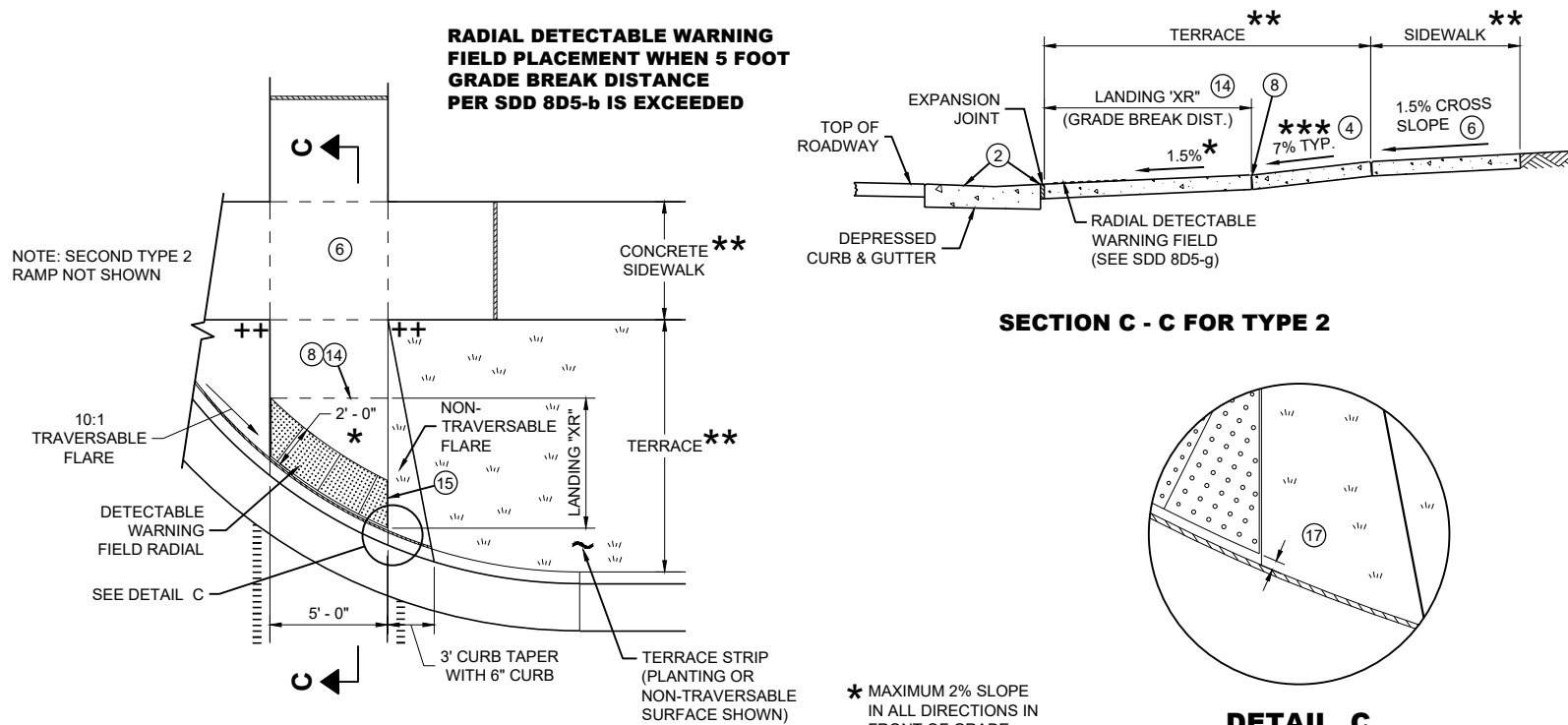


SECTION B - B FOR TYPE 4B1

**** IF RAMP SLOPE IS LESS THAN 5.0%, THEN NO ADJACENT UPHILL LANDING IS REQUIRED

*** MAXIMUM 8.33%

**RADIAL DETECTABLE WARNING
FIELD PLACEMENT WHEN 5 FOOT
GRADE BREAK DISTANCE
PER SDD 8D5-b IS EXCEEDED**



**PLAN VIEW
CURB RAMP TYPE 2
(GRADE BREAK DISTANCE GREATER THAN 5 FEET)
(ON LINE WITH SIDEWALK)**

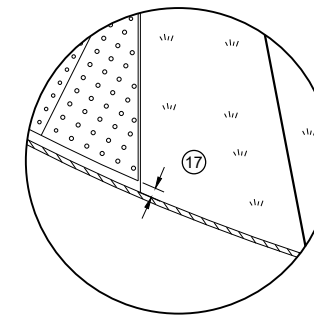
NOTE: SECOND TYPE 2 RAMP NOT SHOWN

* MAXIMUM 2% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK

** WIDTH SHOWN ELSEWHERE IN THE PLANS

*** MAXIMUM 8.33%

++ CONSTRUCT 6" WEDGE TO AVOID CONCRETE BREAKAGE



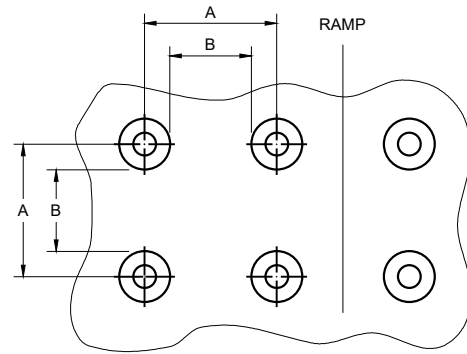
DETAIL C

**CURB RAMPS
RADIAL DETECTABLE WARNING**

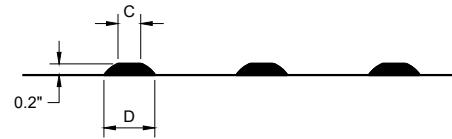
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

	MIN.	MAX.
A	1.6"	2.4"
B	0.65"	1.5"
C	*	*
D	0.9"	1.4"

* THE C DIMENSION IS 50% TO 65% OF THE D DIMENSION.

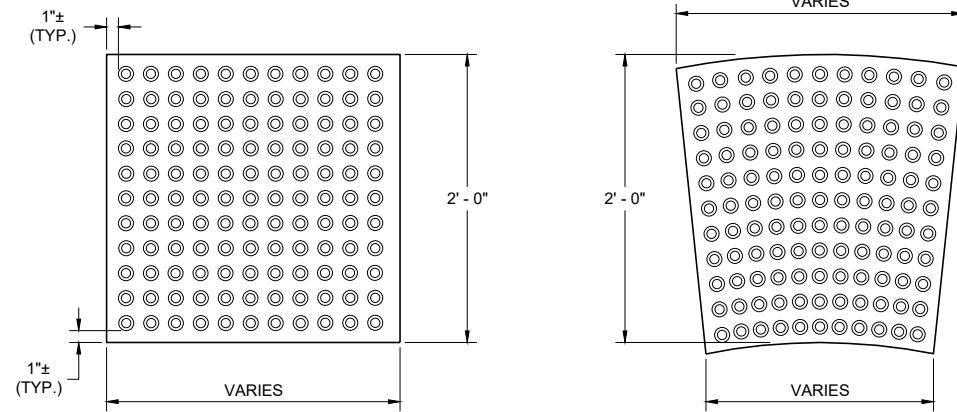


PLAN VIEW



ELEVATION VIEW

**TRUNCATED DOMES
DETECTABLE WARNING PATTERN DETAIL**

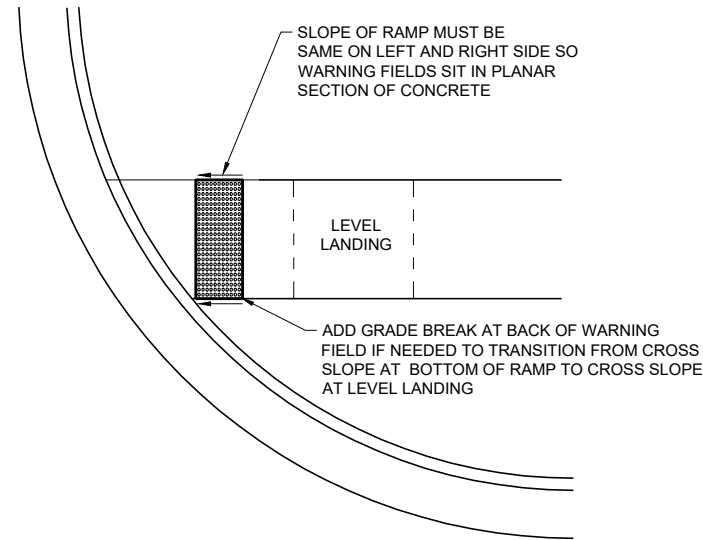


**RECTANGULAR
PLATES**

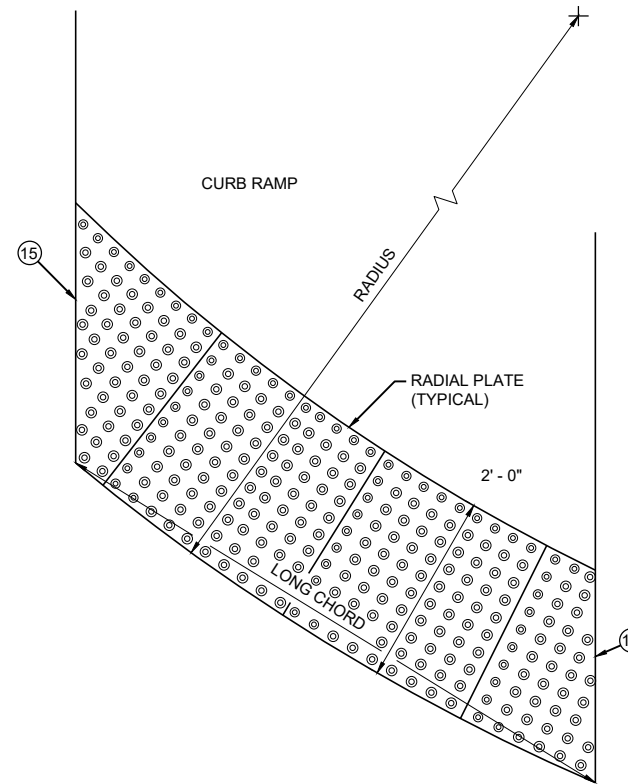
**RADIAL
PLATES**

PLAN VIEW

DETECTABLE WARNING FIELDS (TYPICAL)



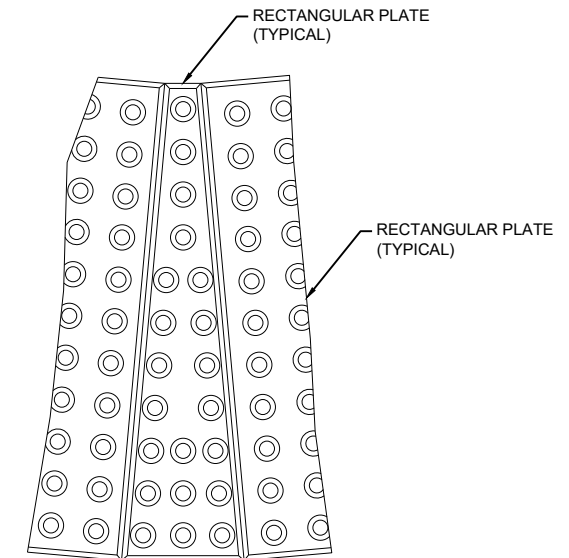
**DETECTABLE WARNING FIELD
PLANAR INSTALLATION**



**PLAN VIEW
RADIAL DETECTABLE
WARNING FIELD ATTRIBUTES**

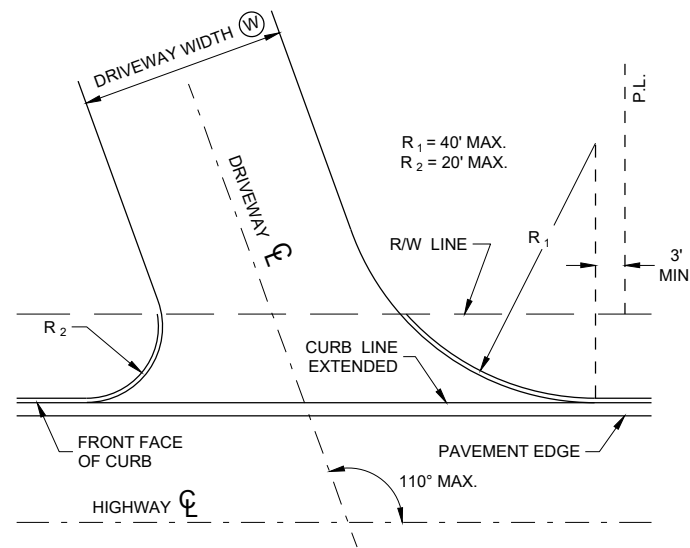
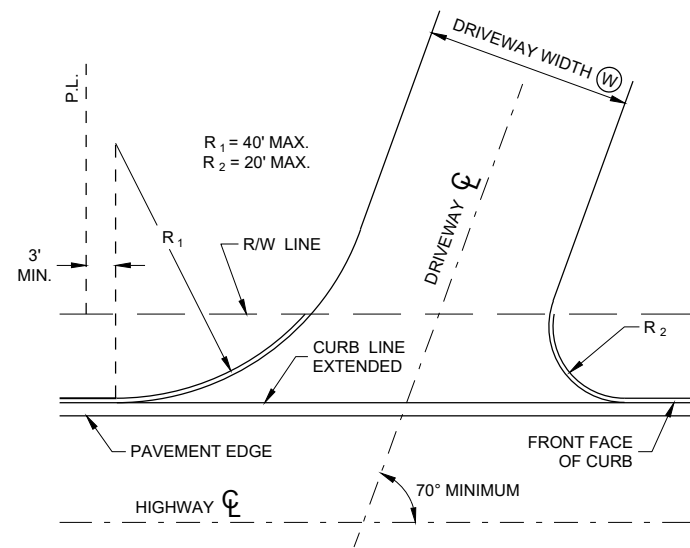
GENERAL NOTES

- DETECTABLE WARNING FIELDS THAT ARE INSTALLED AT A CURB RAMP SHALL BE FROM THE SAME MANUFACTURER.
- PLACE ALL DETECTABLE WARNING FIELD SYSTEMS IN ACCORDANCE TO THE MANUFACTURER'S RECOMMENDATION.
- FIELD CUTS AT INTERMEDIATE JOINTS WITHIN THE RADIAL DETECTABLE WARNING FIELD ARE PROHIBITED.
- DETERMINE FINAL RADIAL WARNING FIELD CONFIGURATION AND ITS INDIVIDUAL PLATE LOCATIONS. PERFORM PRE-LAYOUT PRIOR TO PLACEMENT IN PLASTIC CONCRETE. FOLLOW MANUFACTURER'S PRODUCT LIST AND INSTALLATION RECOMMENDATIONS.
- FOR RADIAL DETECTABLE WARNING FIELD APPLICATIONS WHERE STANDARD RADIAL PLATES ARE NOT AVAILABLE AT AN INTERSECTION CURB RADIUS, A COMBINATION OF SQUARE OR RECTANGULAR PLATES AND RADIAL PLATES MAY BE USED TO FORM RADIAL CONFIGURATION. RADIAL WEDGE PLATES IN COMBINATION WITH SQUARE PLATES ARE ALSO ACCEPTABLE. FOLLOW MANUFACTURER'S RECOMMENDATIONS.
- REFER TO CONTRACT AND STANDARD SPECIFICATIONS FOR FIELD CUTTING REQUIREMENTS.
- DO NOT EMBED IN CONCRETE ANY FIELD-CUT PLATES WITH CUT EDGES SHORTER THAN 6 INCHES. CONSULT WITH MANUFACTURER FOR RE-DRILLING AND ANCHORING REQUIREMENTS OF FIELD-CUT PLATES.
- (15) FIELD SAW CUTS ALONG RADIAL DETECTABLE WARNING PLATES WILL BE NECESSARY TO MATCH EACH CURB RAMP EDGE. AVOID CUTTING THROUGH DOMES WHENEVER POSSIBLE. MAKE FIELD CUTS TRUE TO LINE AND WITHIN 1/8" DEVIATION. SMOOTH EDGES OF FIELD CUT PLATES.



**PLAN VIEW
RADIAL WEDGE PLATE
CONNECTION DETAIL**

CURB RAMPS RECTANGULAR AND RADIAL DETECTABLE WARNING PLATES	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED July 2023 DATE	/s/ Rodney Taylor ROADWAY STANDARDS DEVELOPMENT UNIT SUPERVISOR
FHWA	



**SKewed DRIVEWAY DETAILS
(COMMERCIAL AND NON-COMMERCIAL)
SIDEWALK NOT SHOWN**

GENERAL NOTES

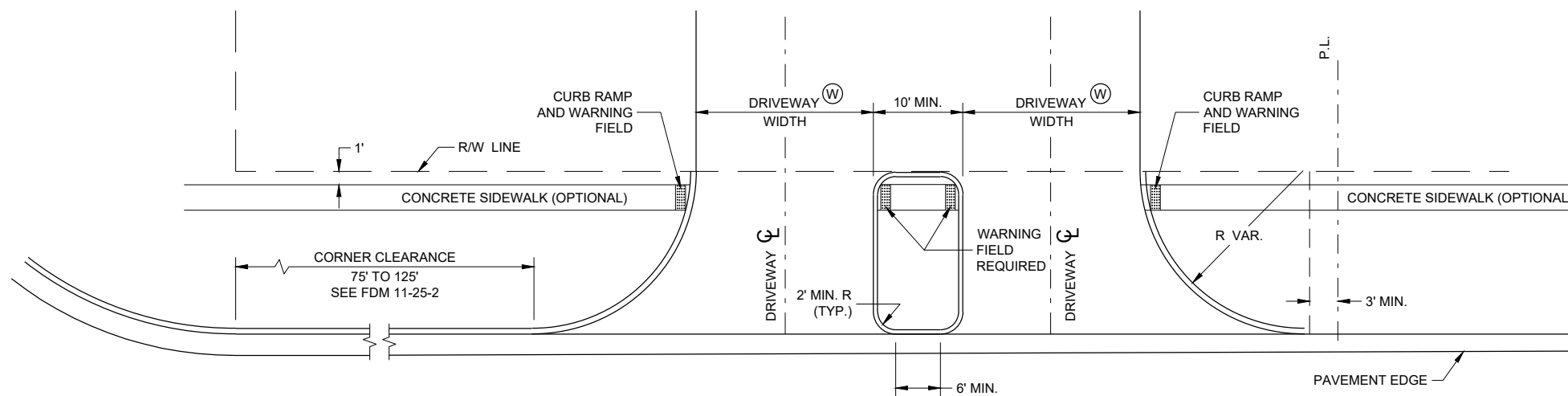
A MAXIMUM RADIUS OF 10 FEET SHALL BE USED FOR NON-COMMERCIAL PRIVATE ENTRANCES. RADII FOR COMMERCIAL DRIVEWAYS SHALL BE DETERMINED BY THE ENGINEER BASED ON TRAFFIC AND DRIVEWAY PERMIT RESTRICTIONS.

THE MINIMUM ANGLE OF INTERSECTION BETWEEN THE DRIVEWAY AND HIGHWAY CENTERLINES SHALL BE 70°.

ALL CURVILINEAR PRIVATE ENTRANCE OUTLINES SHALL BE CONTAINED WITHIN THE HIGHWAY R/W.

NO DRIVEWAY SHALL BE BUILT WITHIN 3 FEET OF THE PROPERTY LINE EXCEPT FOR EXISTING JOINT DRIVEWAY SHARED BY TWO OWNERS.

Ⓜ: 12' MIN. - 24' MAX. RESIDENTIAL AND NON-COMMERCIAL (PE & FE)
16' MIN. - 35' MAX. COMMERCIAL (CE)



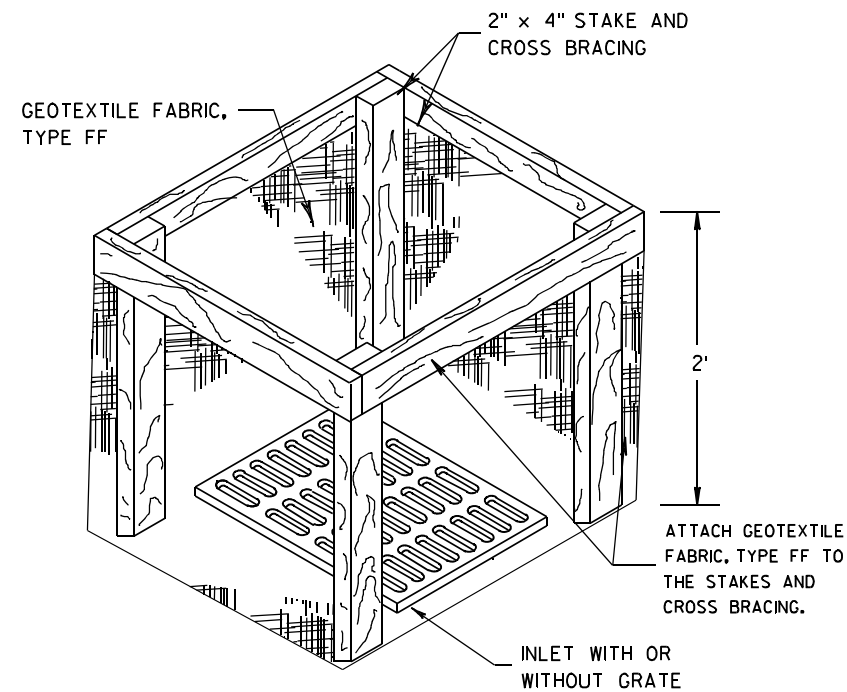
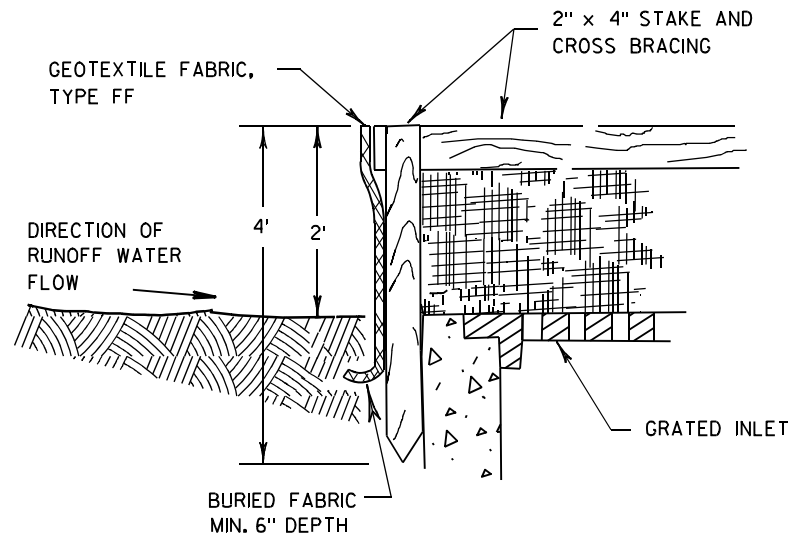
**DRIVEWAY LOCATION AND SPACING DETAILS
SIDEWALK SHOWN**

**DRIVEWAYS WITH
CURB AND GUTTER
RETURNS**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
00-00-00 DATE /S/ <AUTHOR>
ROADWAY STANDARDS DEVELOPMENT ENGINEER

FHWA



INLET PROTECTION, TYPE A

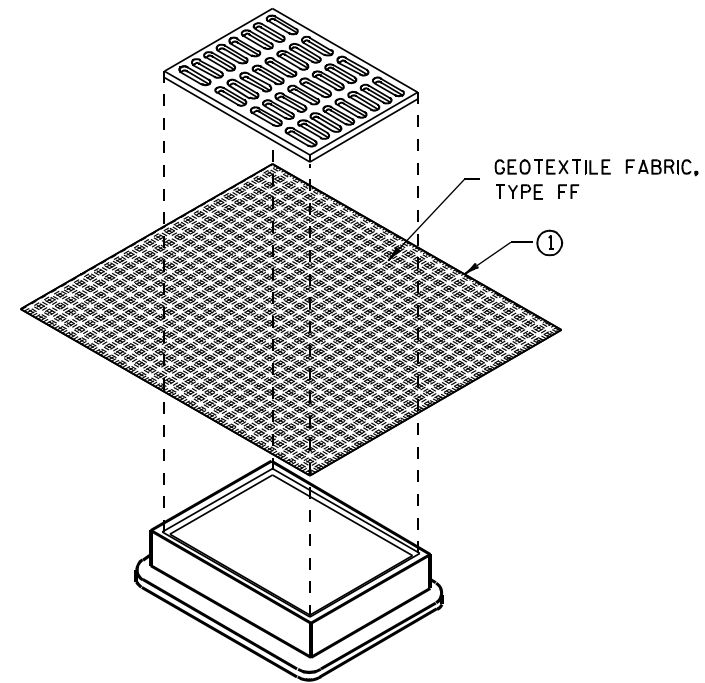
GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

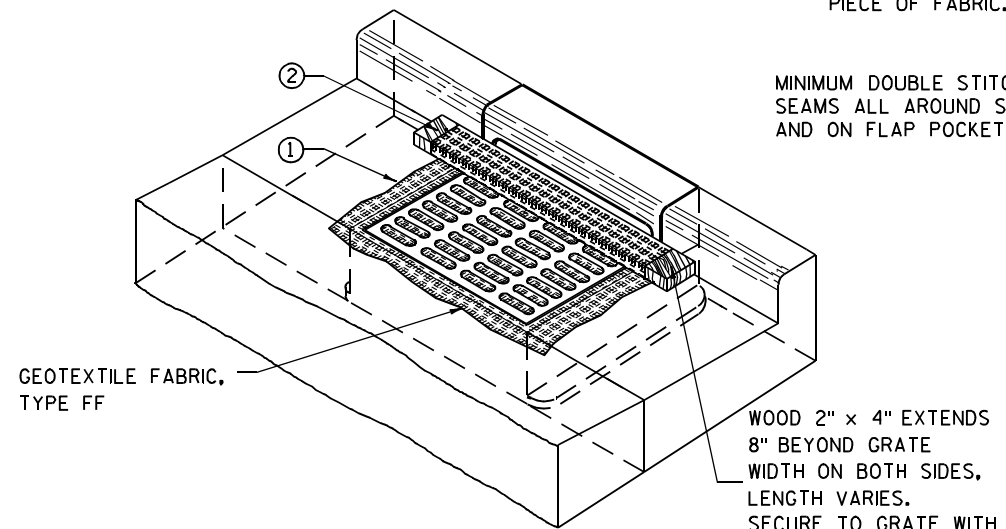
WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- ① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- ② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- ③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



**INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)**

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE C (WITH CURB BOX)

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

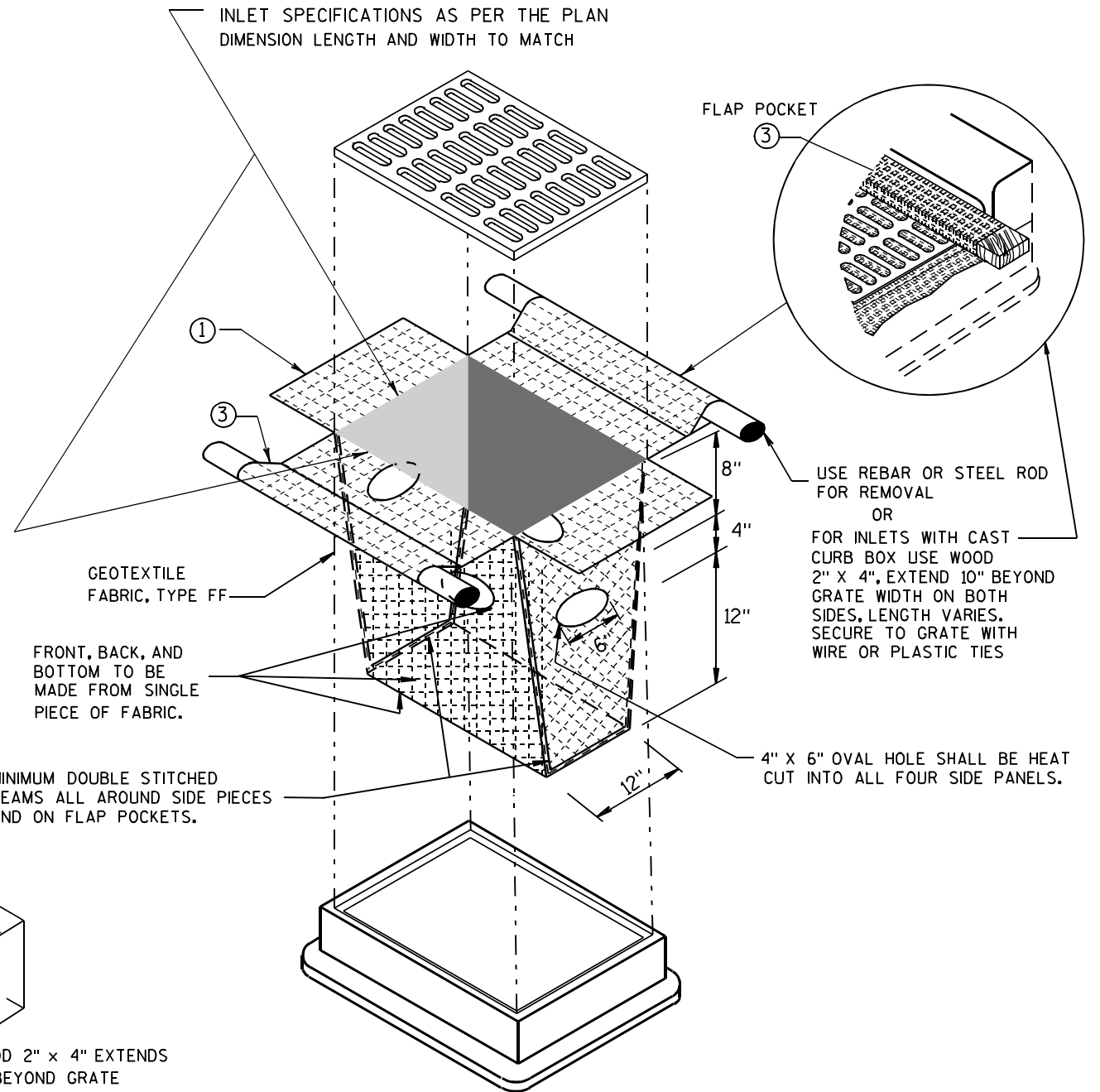
THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.



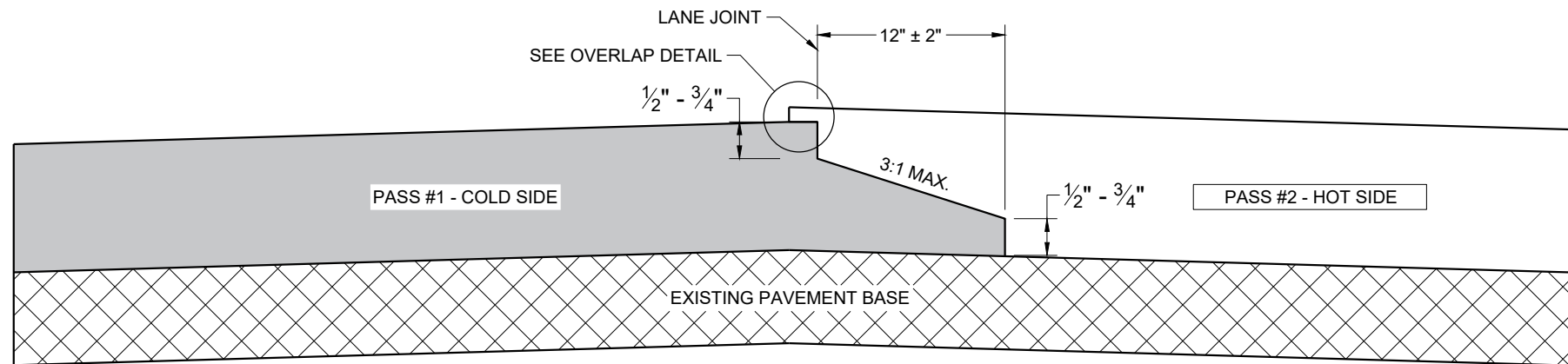
INLET PROTECTION, TYPE D

(CAN BE INSTALLED IN ANY INLET TYPE WITH OR WITHOUT A CURB BOX AS PER NOTE ②)

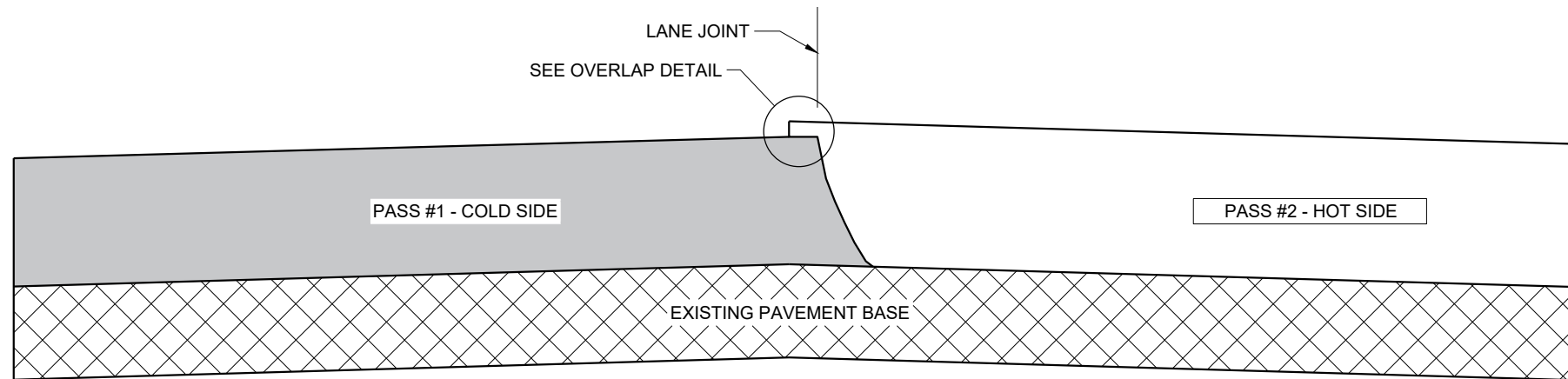
**INLET PROTECTION
TYPE A, B, C, AND D**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

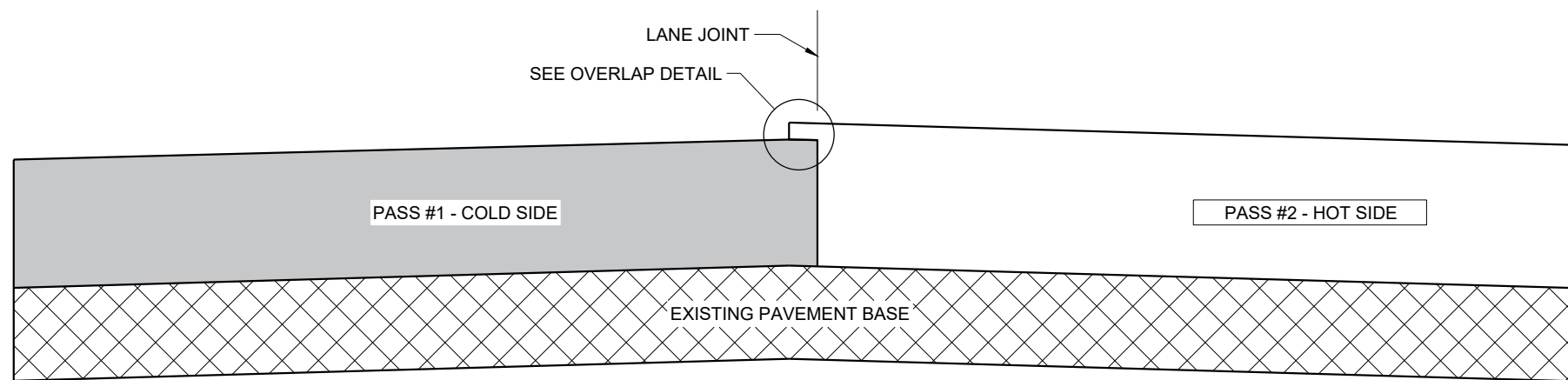
APPROVED
10/16/02 /S/ Beth Connestra
DATE
CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA



**TYPICAL PAVEMENT CROSS SECTION
NOTCHED WEDGE JOINT**



**TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT**



**TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT (MILLED)**

GENERAL NOTES

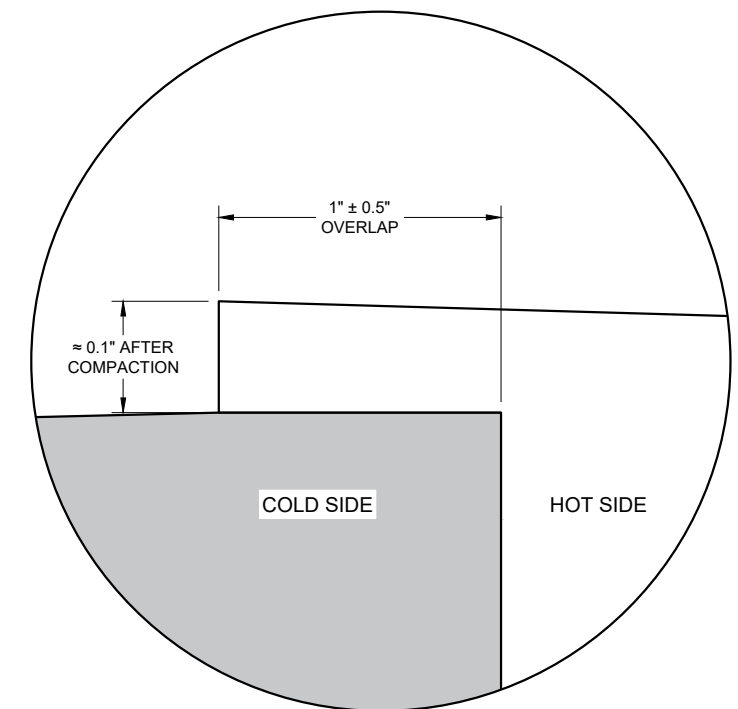
IN ADDITION TO THE DETAILS PROVIDED IN THIS DRAWING, CONFORM TO STANDARD SPECIFICATION 450.3.2.8 FOR WHEN A NOTCHED WEDGE JOINT IS REQUIRED AND FOR GENERAL JOINT CONSTRUCTION REQUIREMENTS.

FOR ALL LONGITUDINAL JOINTS, ENSURE THE PAVER SCREED OVERLAPS THE PREVIOUSLY PLACED PAVEMENT BY $1" \pm 0.5"$ AND THE HOT SIDE OF THE JOINT REMAINS HIGHER THAN THE COLD SIDE BY APPROXIMATELY 0.1" AFTER FINAL COMPACTION. (IT WILL BE FLUSH WHEN PAVING IN ECHELON.)

ONLY REMOVE THE LONGITUDINAL NOTCHED WEDGE JOINT FOR SMA PAVEMENT OR AS DIRECTED BY THE ENGINEER TO ADDRESS SPECIFIC LENGTHS OF JOINT DAMAGED BY TRAFFIC.

WHEN MILLING BACK OR REMOVING ANY LONGITUDINAL JOINT, LIMIT THE MATERIAL REMOVED TO 2" FROM THE TOP NOTCH OR FROM THE VERTICAL JOINT EDGE ON THE COLD SIDE OF THE JOINT.

USE LONGITUDINAL MILLED JOINT AS PLANS SHOW OR THE AS THE ENGINEER DIRECTS.



OVERLAP DETAIL (TYPICAL)

6

6

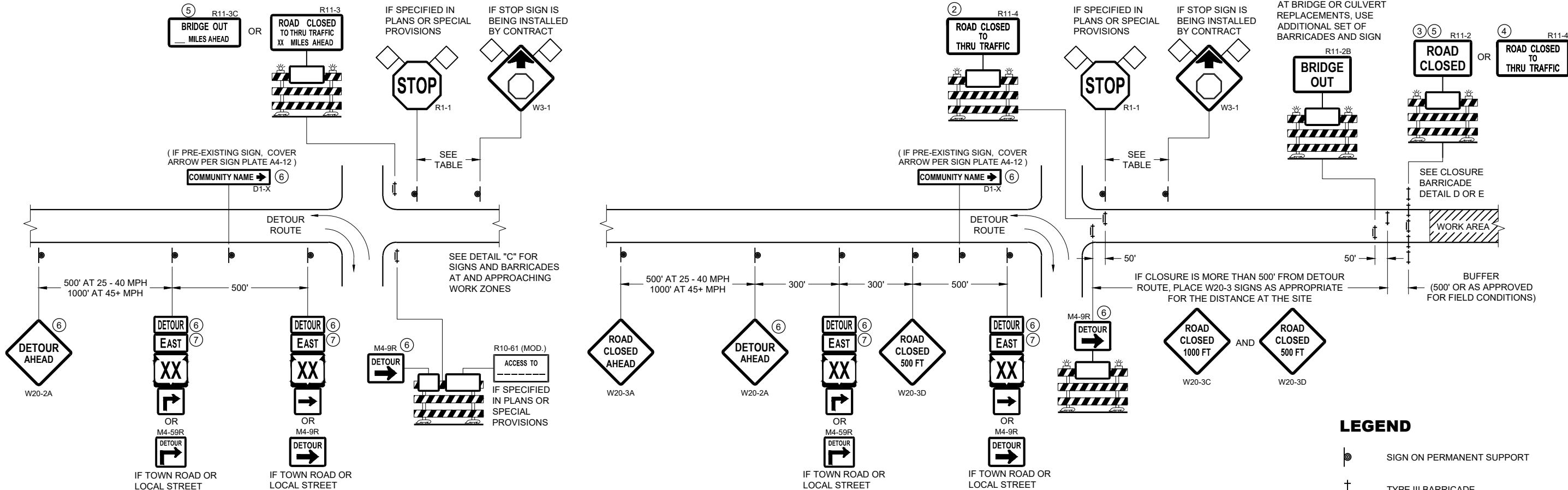
SDD 13C19 - 03

SDD 13C19 - 03

HMA LONGITUDINAL JOINTS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2020 DATE /S/ Steven Hefel
HMA PAVEMENT ENGINEER
FHWA



**DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR**

WORK ZONE GREATER THAN OR EQUAL TO 1/2 MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)

**DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR**

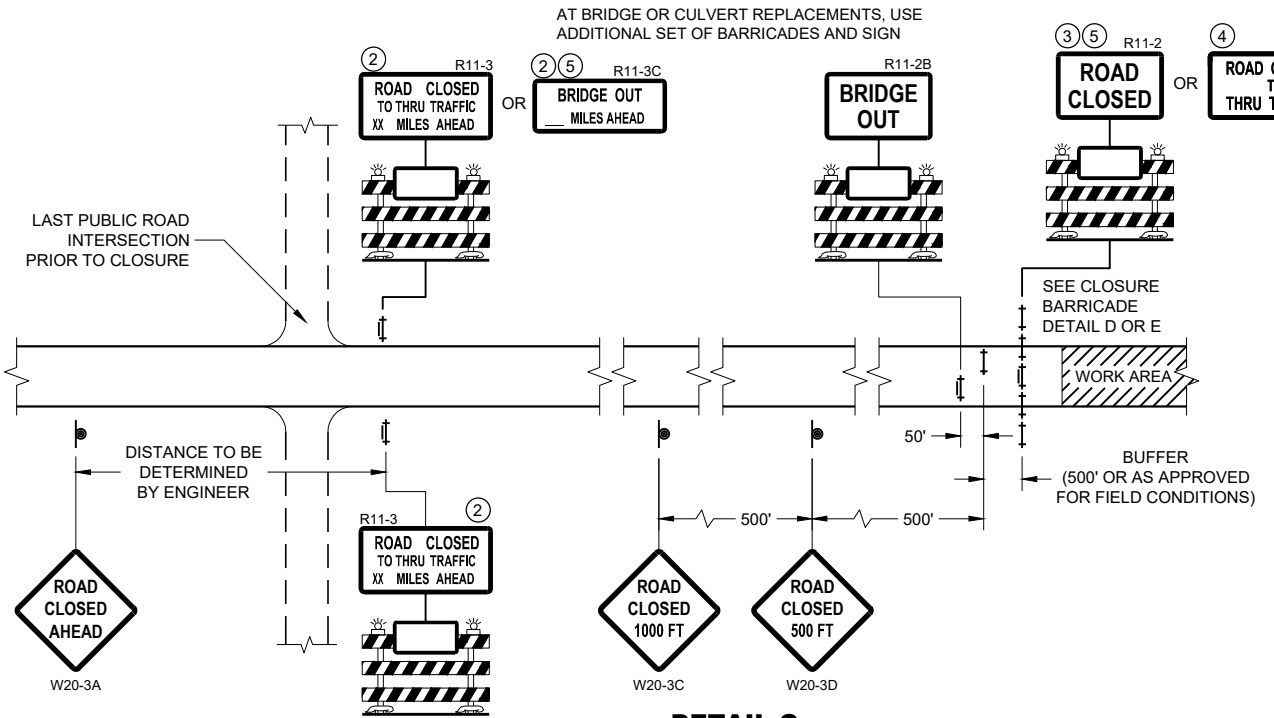
WORK ZONE LESS THAN 1/2 MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
- FLAGS, 16" X 16" MIN. (ORANGE)

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750

- M4 - 8
- M3 - X
- M1 - 4
- M1 - 6
- M1 - 5A
- M05 - 1
- M06 - 1



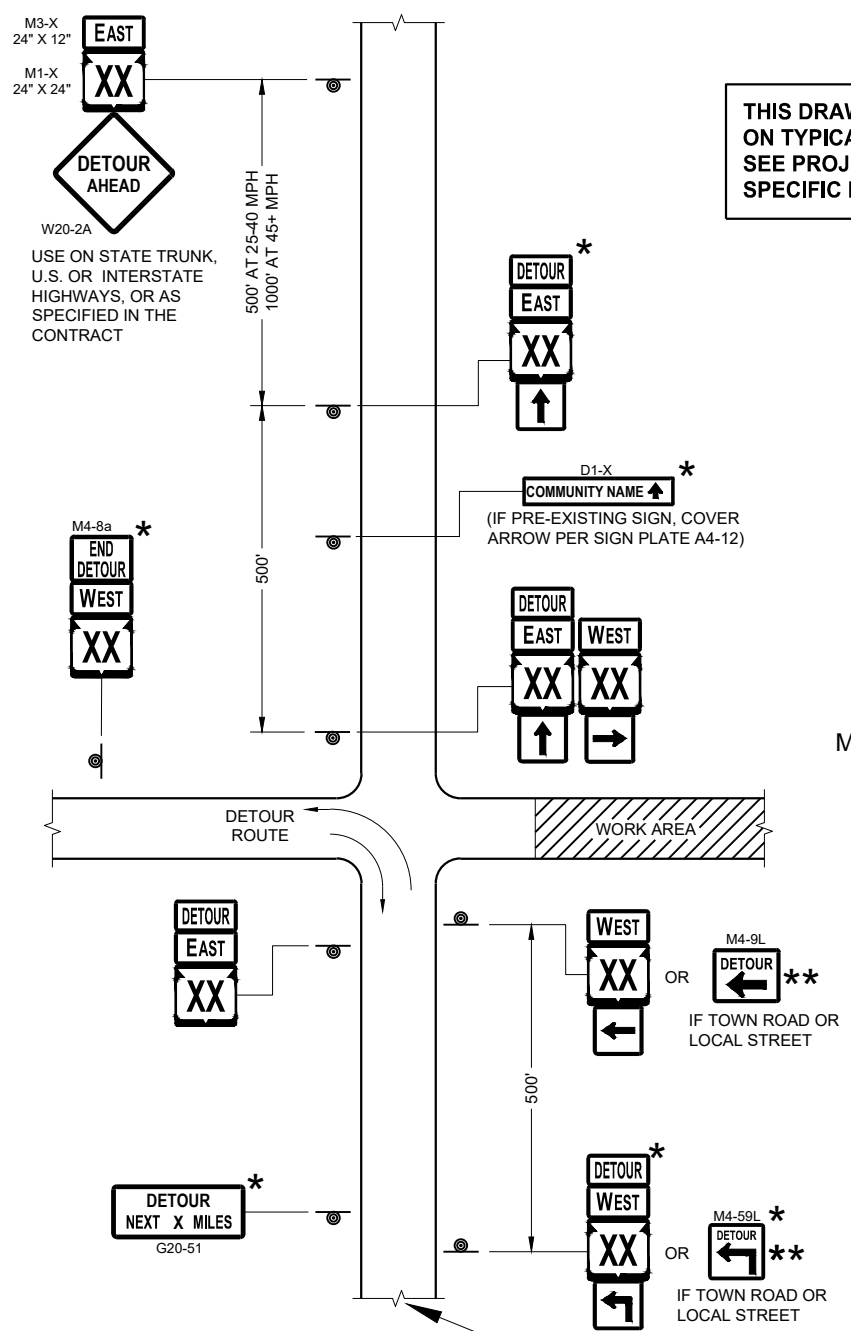
**DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR**

SEE SDD 15C2-SHEET "b"
FOR GENERAL NOTES
AND FOOTNOTES ① THROUGH ⑦

**BARRICADES AND SIGNS
FOR MAINLINE CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 /S/ Andrew Heidtke
DATE DATE WORK ZONE ENGINEER



THIS DRAWING PROVIDES GENERAL GUIDANCE ON TYPICAL DETOUR SIGN LAYOUT AND SPACING. SEE PROJECT DETOUR SIGNING SHEETS FOR SPECIFIC DETAILS FOR EACH PROJECT.

LEGEND

- SIGN ON PERMANENT SUPPORT
- WORK AREA
- M4 - 8
- M3 - X
- M1 - 4
- M1 - 6
- M1 - 5A
- M05 - 1
- M06 - 1
- M06 - 1

GENERAL NOTES

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. MODIFY EXISTING SIGNS WHERE POSSIBLE.

THE SPACING BETWEEN TRAFFIC CONTROL AND DETOUR SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

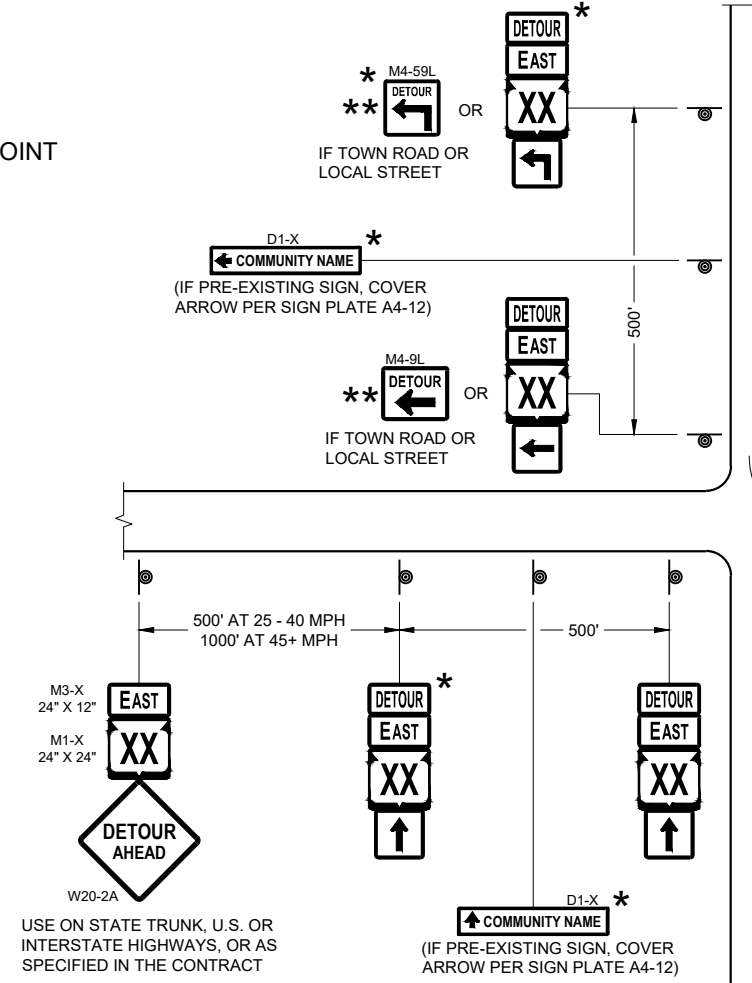
"MO" SIGNS ARE THE SAME AS "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

SIGN SIZES SHALL BE AS FOLLOWS:

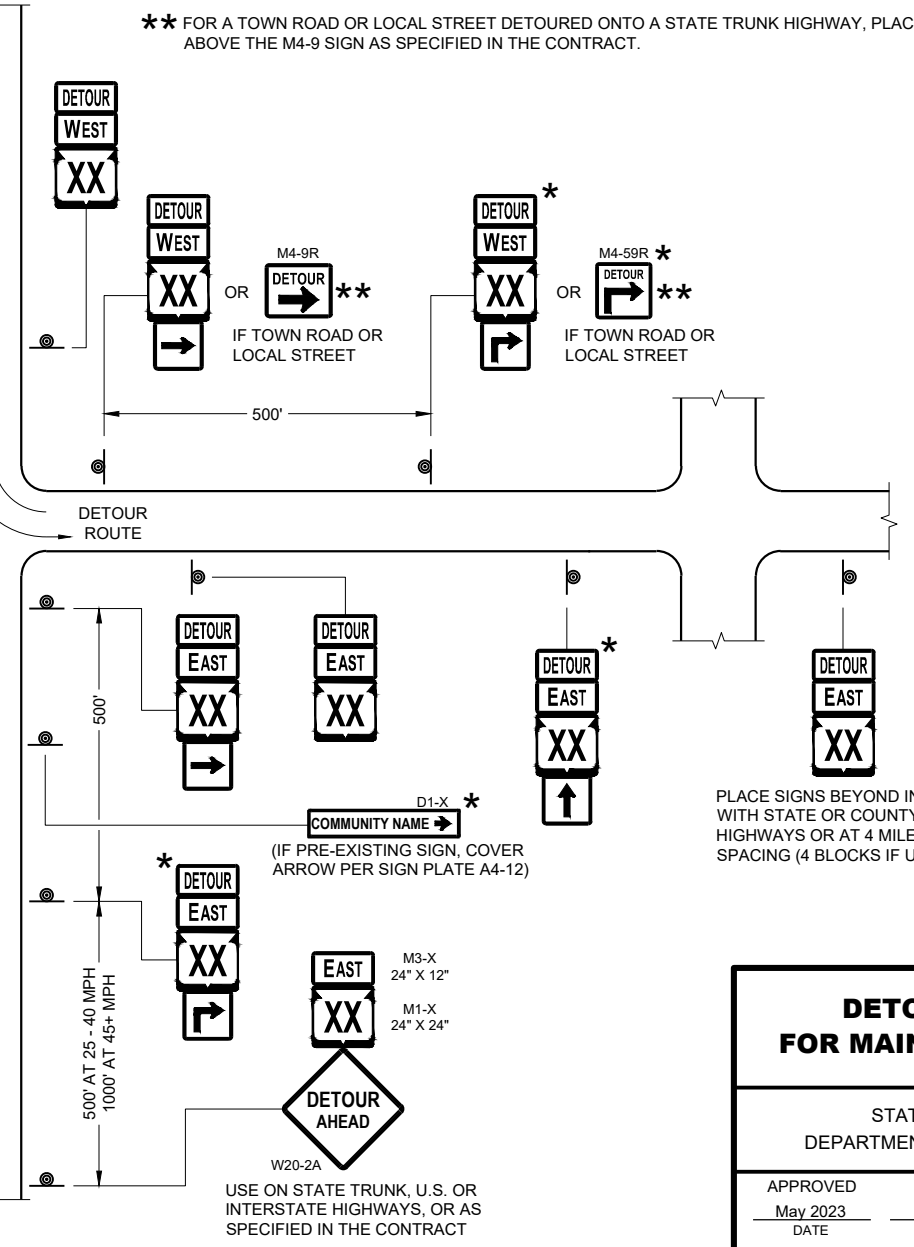
- M3-X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4-8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1-4, M1-5A AND M1-6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- M05-1 AND M06-1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- M4-9 AND M4-59 SHALL BE 30" X 24"
- M4-8a SHALL BE 24" X 18"
- G20-51 SHALL BE 60" X 24"
- W20-2A SHALL BE 48" X 48"
- D1-X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.

- * OPTIONAL SIGNS. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS.
- ** FOR A TOWN ROAD OR LOCAL STREET DETOURED ONTO A STATE TRUNK HIGHWAY, PLACE A ROAD NAME PLAQUE ABOVE THE M4-9 SIGN AS SPECIFIED IN THE CONTRACT.

MATCH POINT



**DETAIL F
DETOUR SIGNING**



**DETOUR SIGNING
FOR MAINLINE CLOSURES**

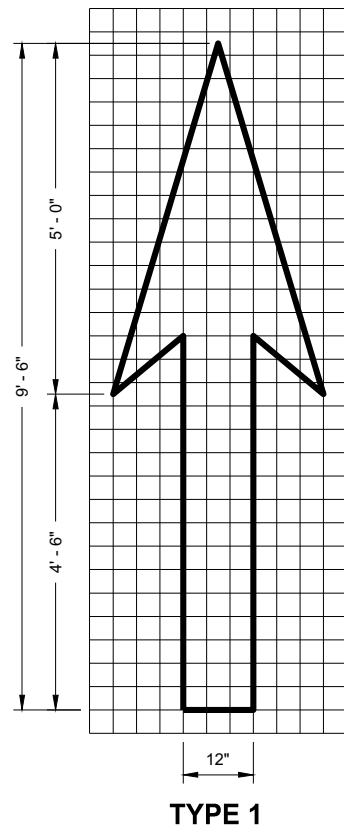
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

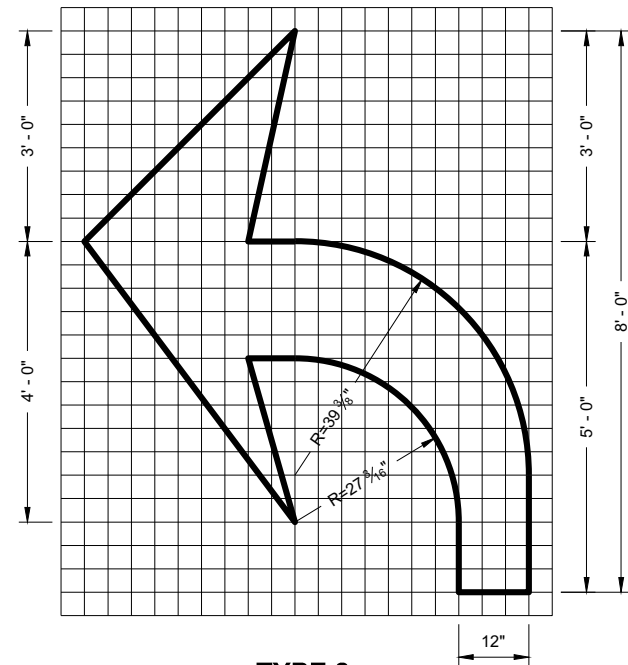
FHWA

SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS AND DETAIL A OR B ON SDD SHEET 15C02 - SHEET "a"

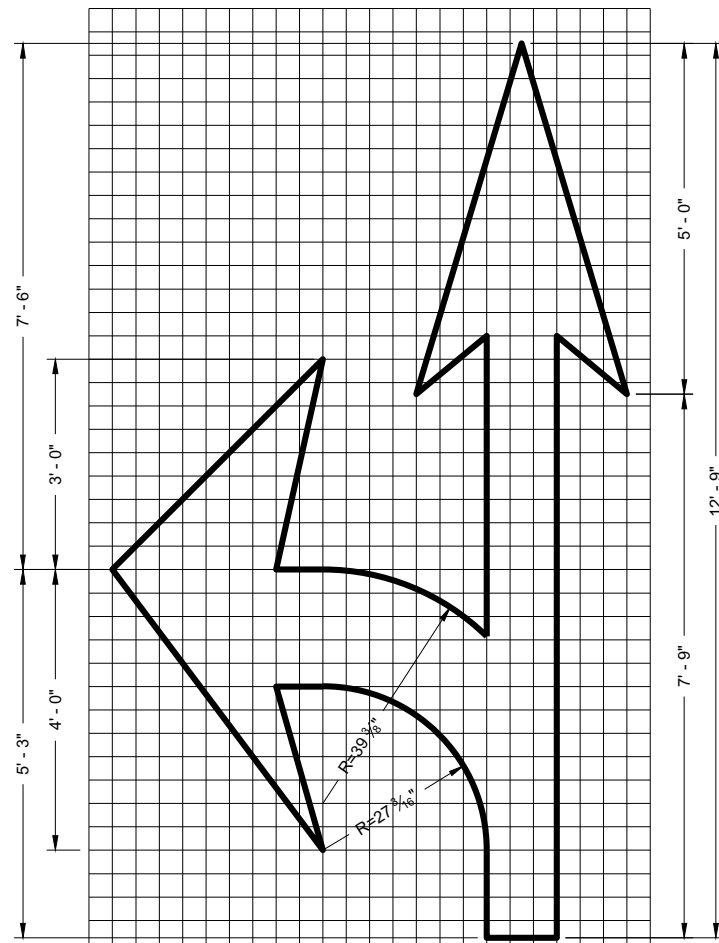
PLACE SIGNS BEYOND INTERSECTIONS WITH STATE OR COUNTY TRUNK HIGHWAYS OR AT 4 MILE MAXIMUM SPACING (4 BLOCKS IF URBAN AREA)



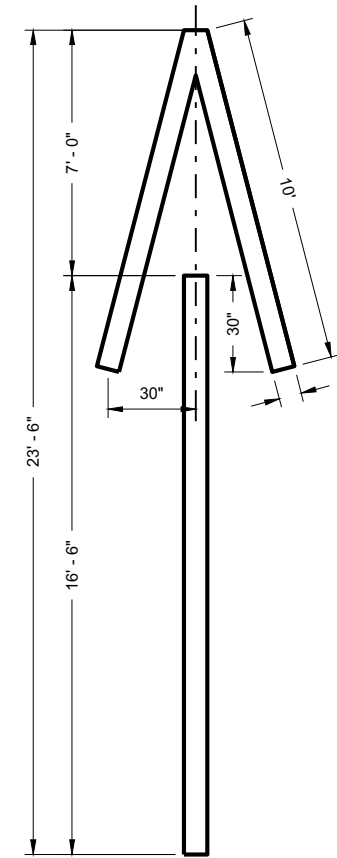
TYPE 1



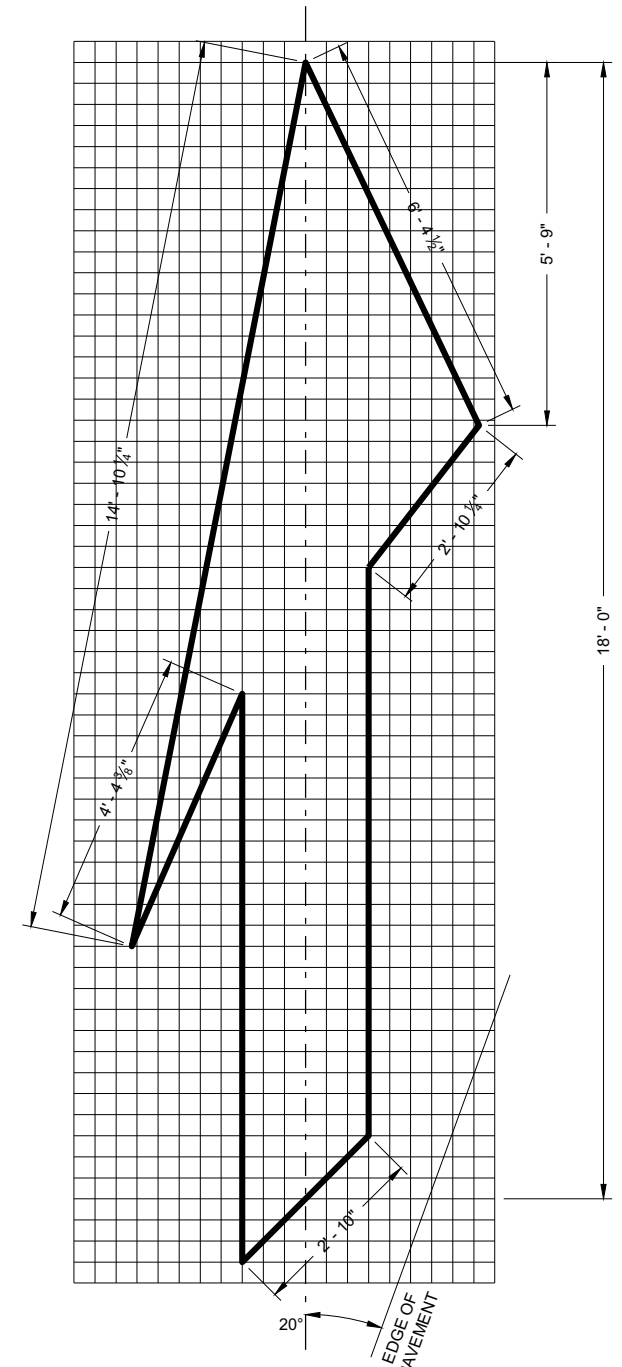
TYPE 2



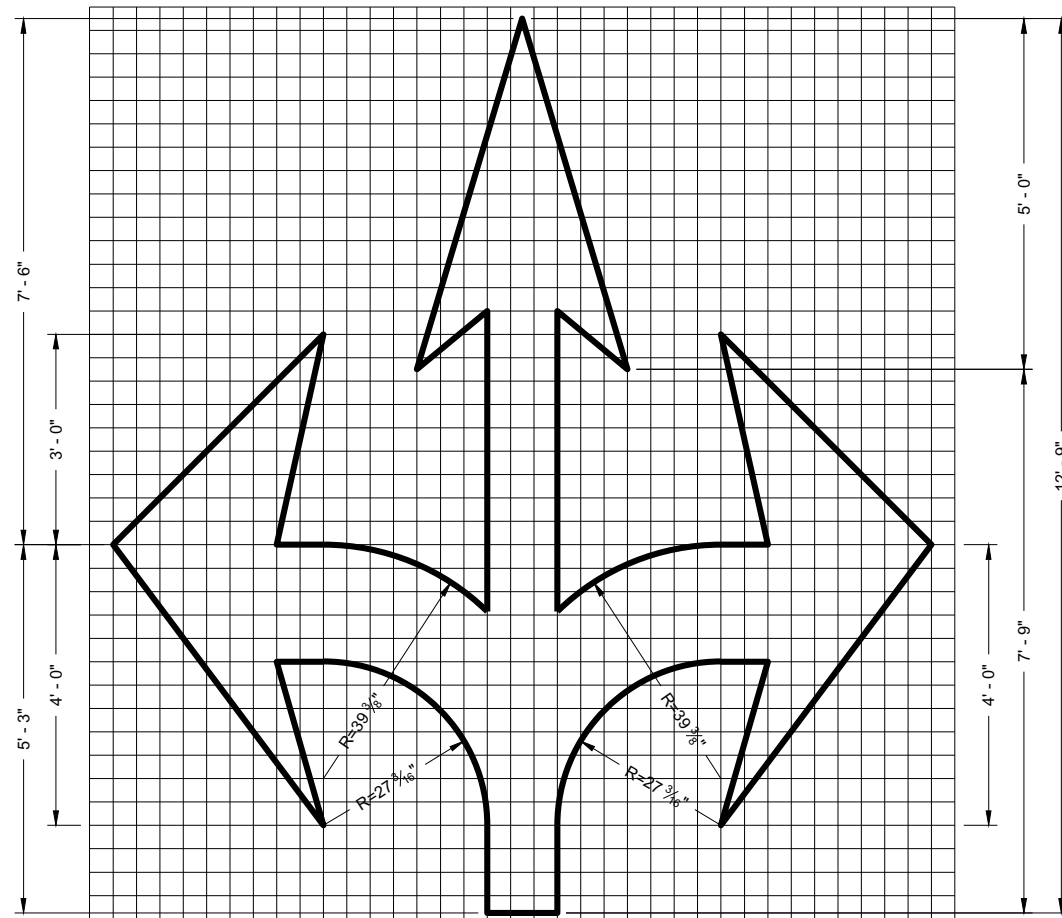
TYPE 3



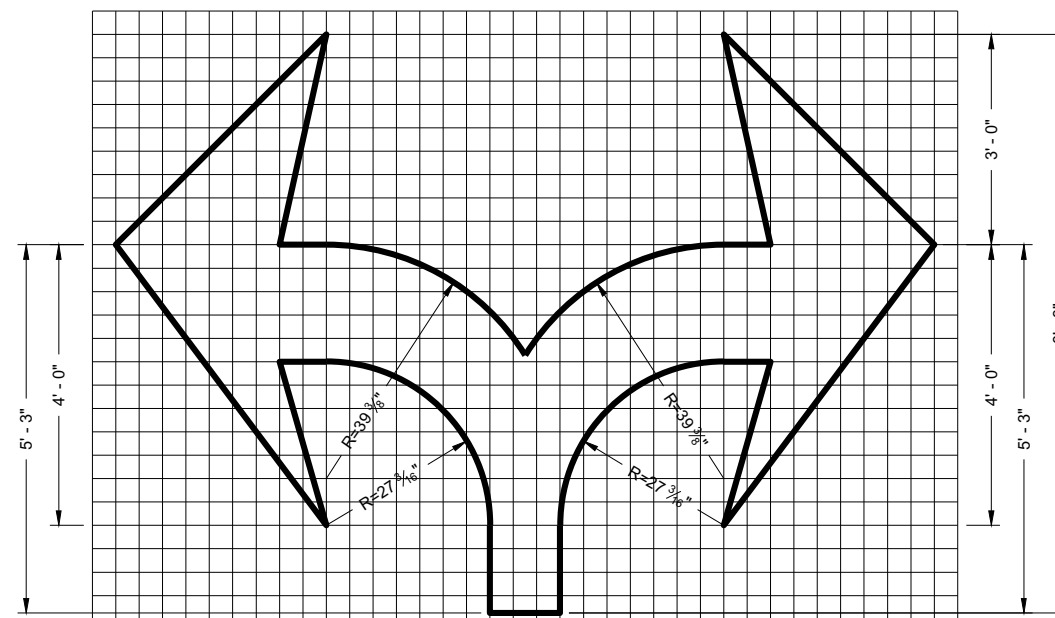
TYPE 4



TYPE 5 LANE DROP ARROW



TYPE 6



TYPE 7

GENERAL NOTES

DETAILS OF INSTALLATION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

PAVEMENT MARKING ARROWS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2019
DATE /S/ Matthew Rauch
STATE SIGNING AND MARKING
ENGINEER
FHWA

GENERAL NOTES

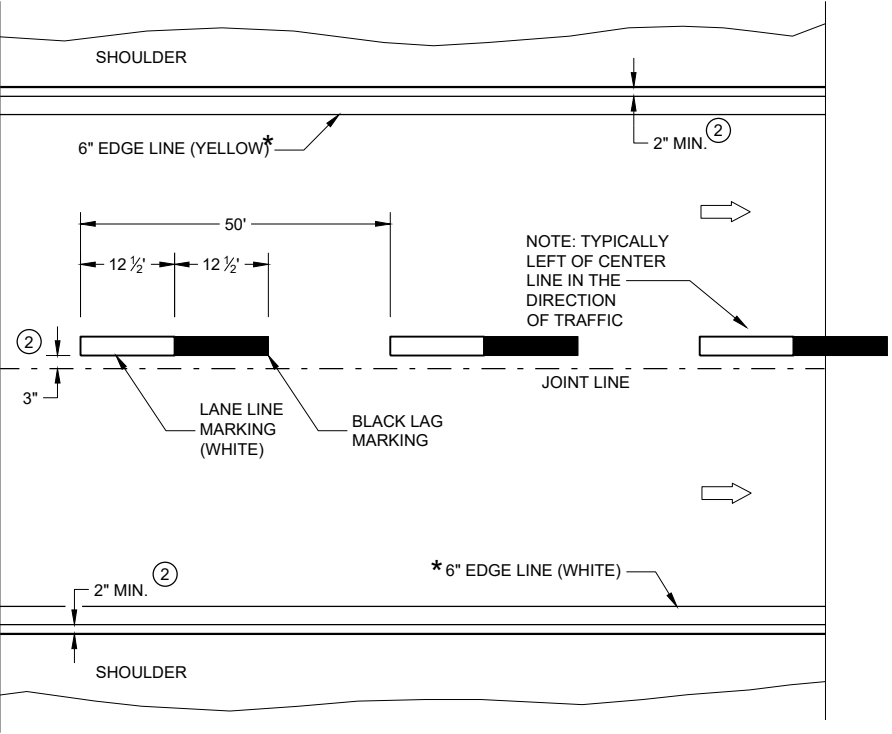
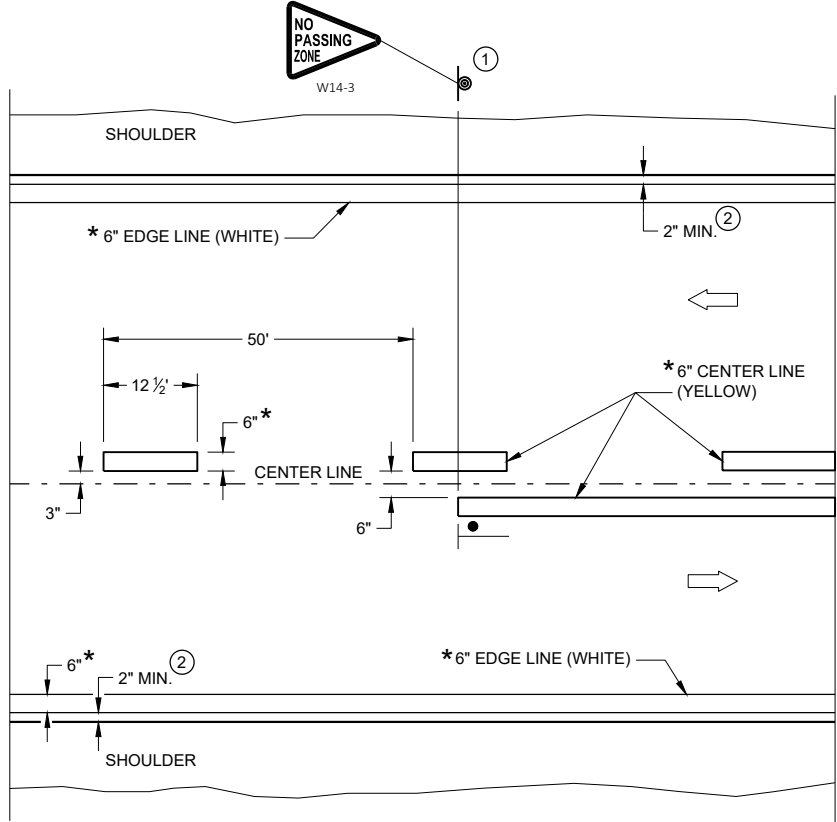
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS.

- ① LOCATE THE NO PASSING ZONE W14-3 SIGN WITHIN 50 FEET OF THE "T" MARKING
- ② MEASURE FROM EDGE OF MARKING TO JOINT LINE. THIS DOES NOT INCLUDE SPACE NEEDED FOR GROOVING OPERATIONS.

LEGEND

- "T" MARKING
- SIGN ON PERMANENT SUPPORT
- DIRECTION OF TRAFFIC

* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES



TWO WAY TRAFFIC

ONE WAY TRAFFIC

PERMANENT PAVEMENT MARKING

PERMANENT LONGITUDINAL PAVEMENT MARKINGS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

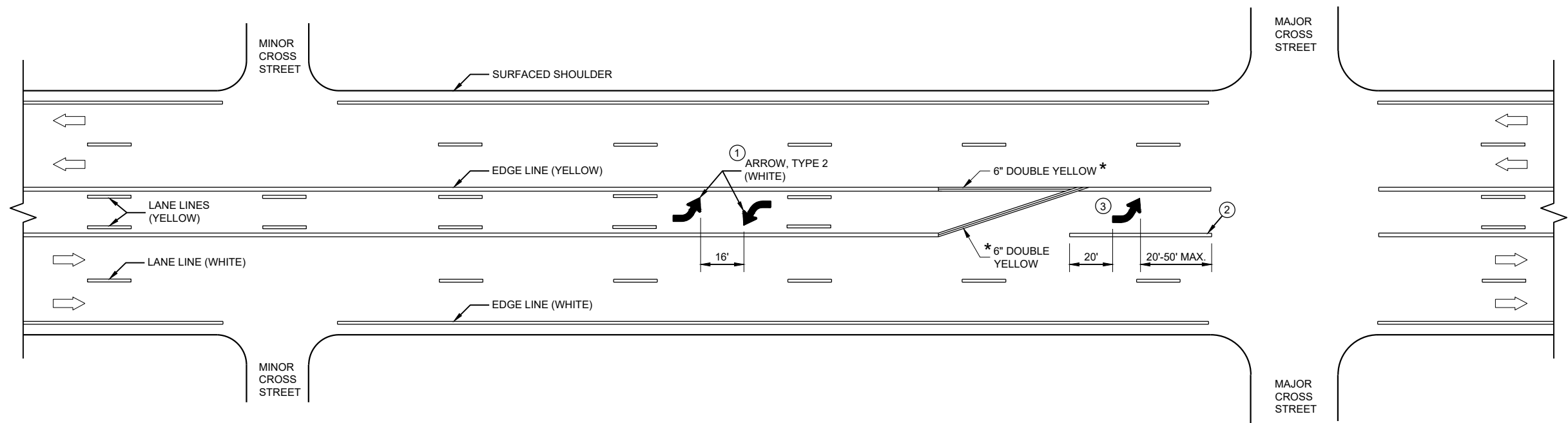
APPROVED
May 2023 /S/ Jeannie Silver
DATE STATEWIDE SIGNING AND MARKING
ENGINEER
FHWA

GENERAL NOTES

- ① A SET OF ARROWS IS REQUIRED EVERY 400 FEET OR NEAR INTERSECTIONS OR DRIVEWAYS WITH TURNING TRAFFIC.
- ② 10" WHITE
- ③ TURN BAY LENGTH OF LESS THAN 48' DOES NOT REQUIRE PAVEMENT ARROWS OR TEXT.

➡ DIRECTION OF TRAFFIC

* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES



TWO WAY LEFT TURN LANE

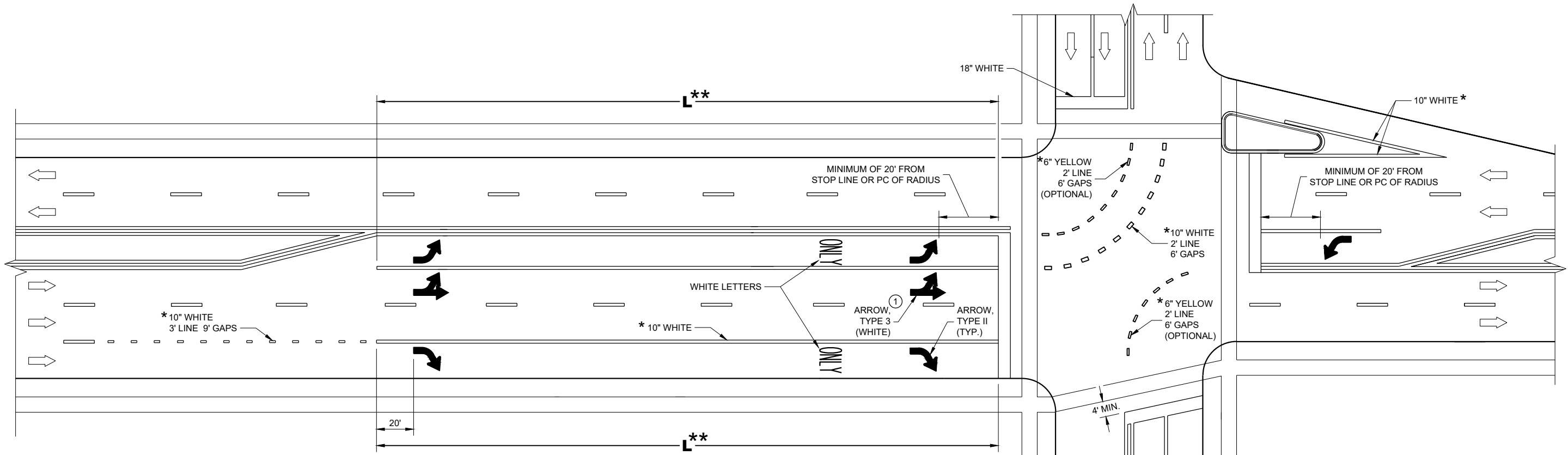
6

6

SDD 15C08-23C

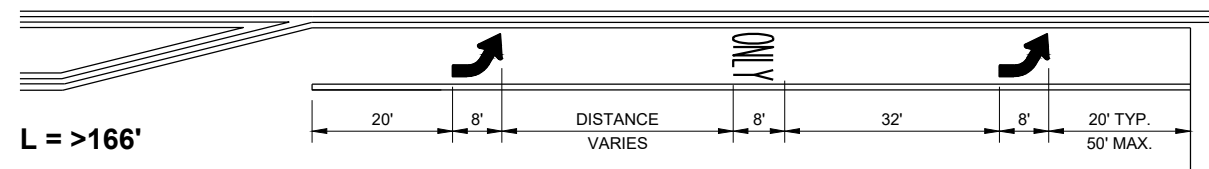
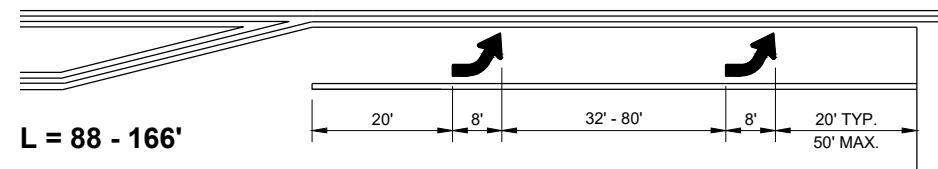
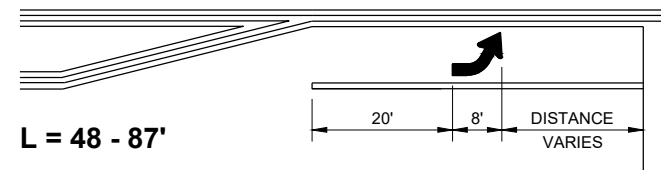
SDD 15C08-23C

PAVEMENT MARKING (TURN LANES)
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION



TURN LANE OPTIONS

LENGTH OF TURN BAY (**L**) OF 0 - 47' DOES NOT REQUIRE PAVEMENT MARKING ARROWS OR WORDS



** (SEE TURN LANE OPTIONS FOR PLACEMENT OF PAVEMENT MARKING ARROWS AND WORDS)

GENERAL NOTES

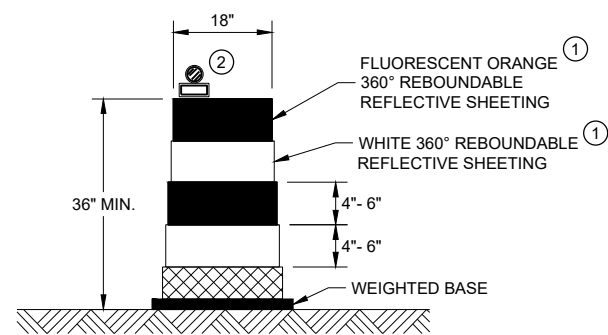
① QUANTITY AND LOCATION OF TYPE 3 ARROWS ARE THE SAME AS THE TYPE II ARROWS IN THE ADJACENT TURN LANE. FOR TURN LANES WITH A PHYSICAL SEPARATION IN THE SAME DIRECTION OF TRAVEL, THE ARROWS AND "ONLY" MARKING MAY BE ELIMINATED.

➡ DIRECTION OF TRAFFIC

L = LENGTH OF TURN BAY

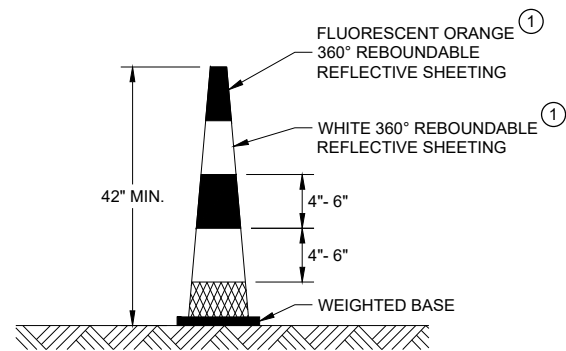
* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES

PAVEMENT MARKING (TURN LANES)
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION



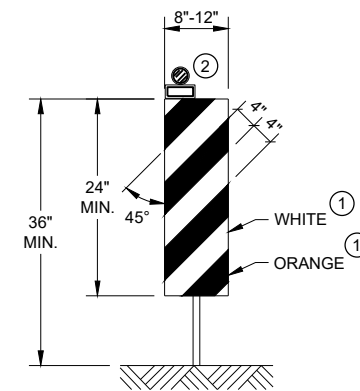
DRUM

BALLAST WIDTHS
RANGE FROM 24"-36"



42" CONE

DO NOT USE IN TAPERS
½ SPACING OF DRUMS
BALLAST WIDTHS
RANGE FROM 14"-20"

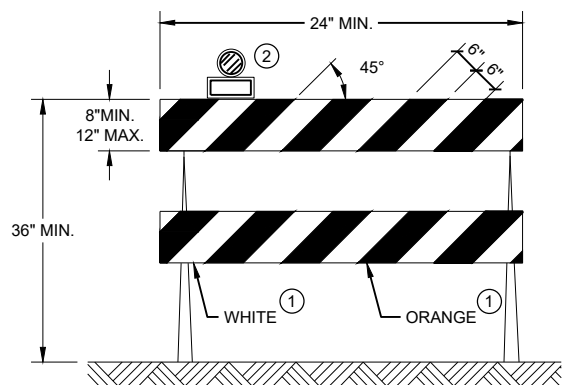


VERTICAL PANEL

THE STRIPES SHALL SLOPE DOWNWARD TO
THE TRAFFIC SIDE FOR CHANNELIZATION.

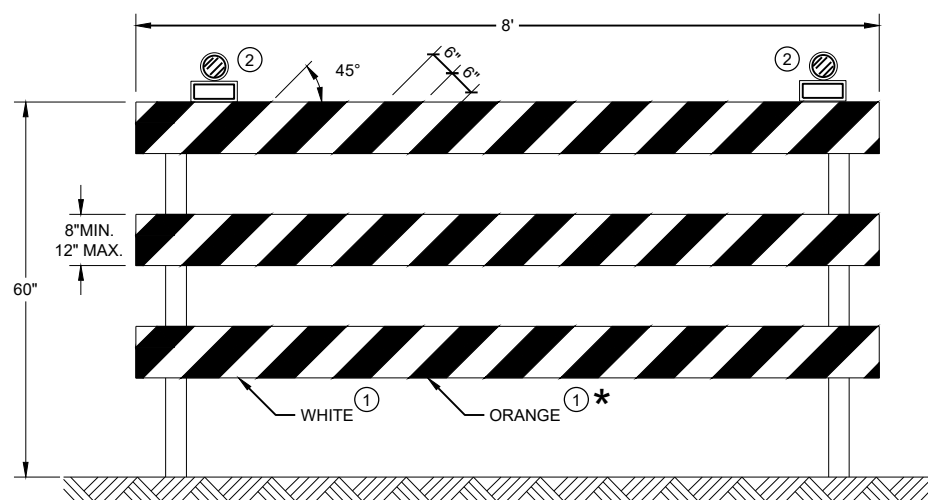
GENERAL NOTES

- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.



TYPE II BARRICADE

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES
MAY BE USED. ALL STRIPES SHALL SLOPE DOWNWARD
TO THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE III BARRICADE

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP
TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.

* IF USED FOR A PERMANENT APPLICATION USE RED SHEETING.

CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED November 2022 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

GENERAL NOTES

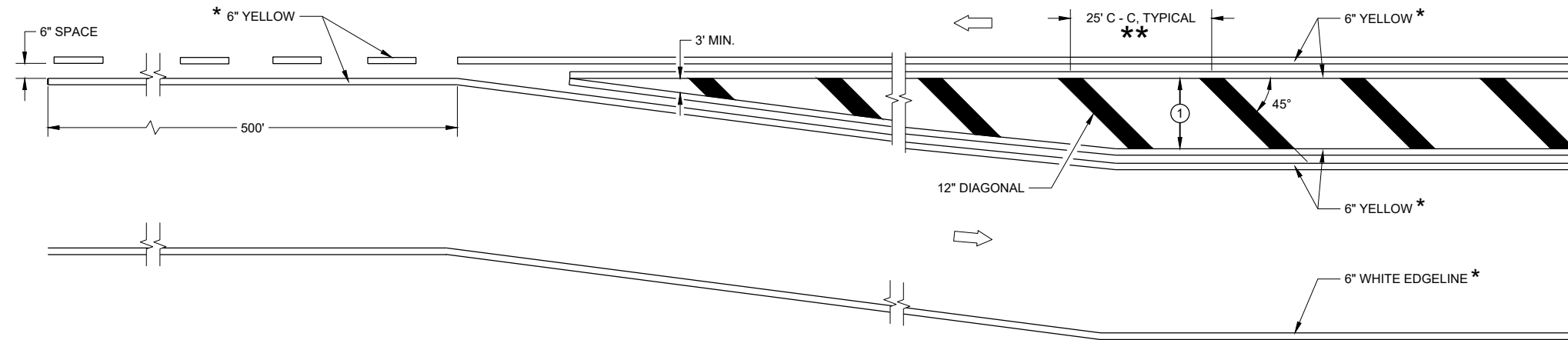
- ① DIAGONALS ARE OPTIONAL WHEN PAINTED ISLAND IS LESS THAN 6 FEET AT THE WIDEST POINT. OMIT DIAGONALS IF WIDTH IS LESS THAN 4 FEET.

➔ DIRECTION OF TRAVEL

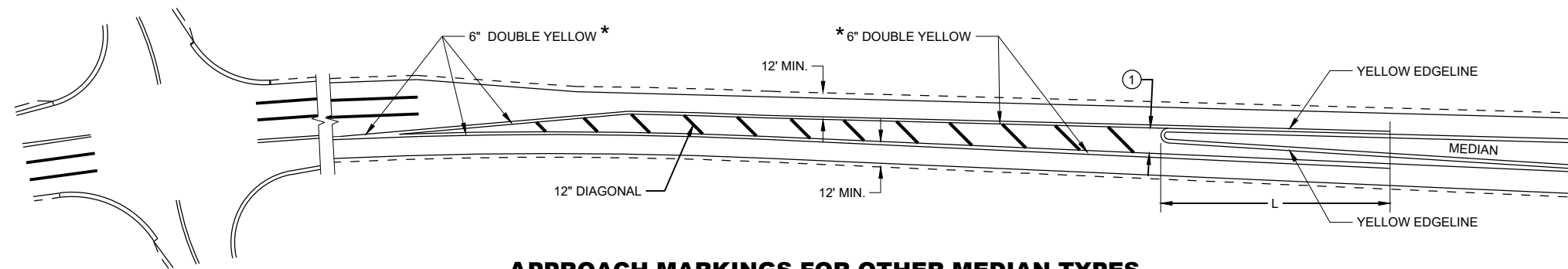
* CONFIRM MARKING LINE WIDTH WITH THE MISCELLANEOUS QUANTITIES

SPEED LIMIT	L
<35 MPH	5'
35> MPH	50'

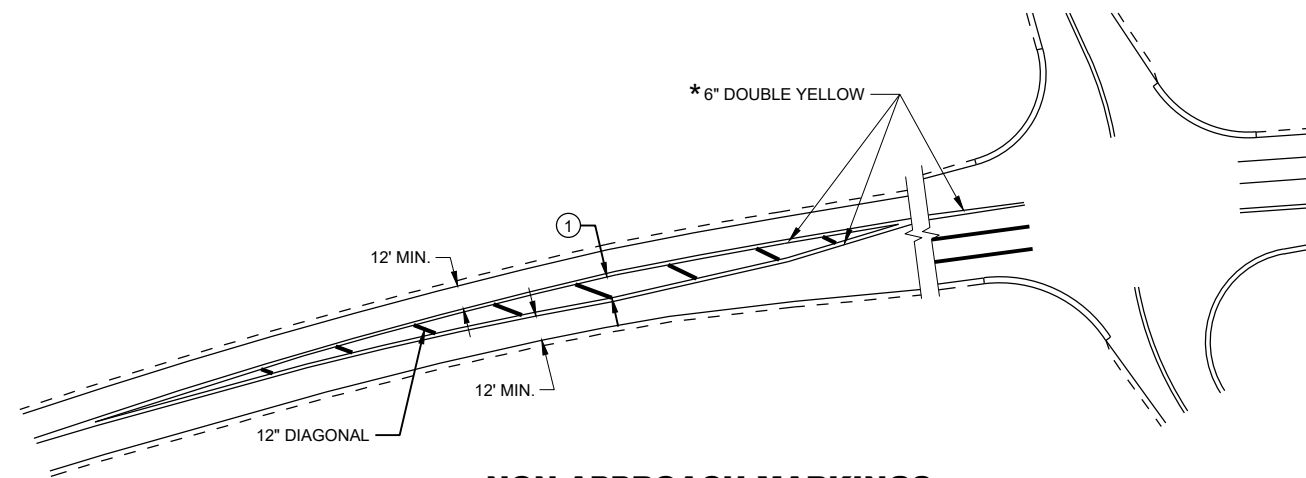
** WHEN THE PAINTED MEDIAN LENGTH IS LESS THAN 50 FEET THE SPACING IS 10'.



MEDIAN ISLAND DETAIL



APPROACH MARKINGS FOR OTHER MEDIAN TYPES



NON-APPROACH MARKINGS

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SDD 15C18-08a

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MEDIAN ISLAND PAVEMENT MARKINGS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

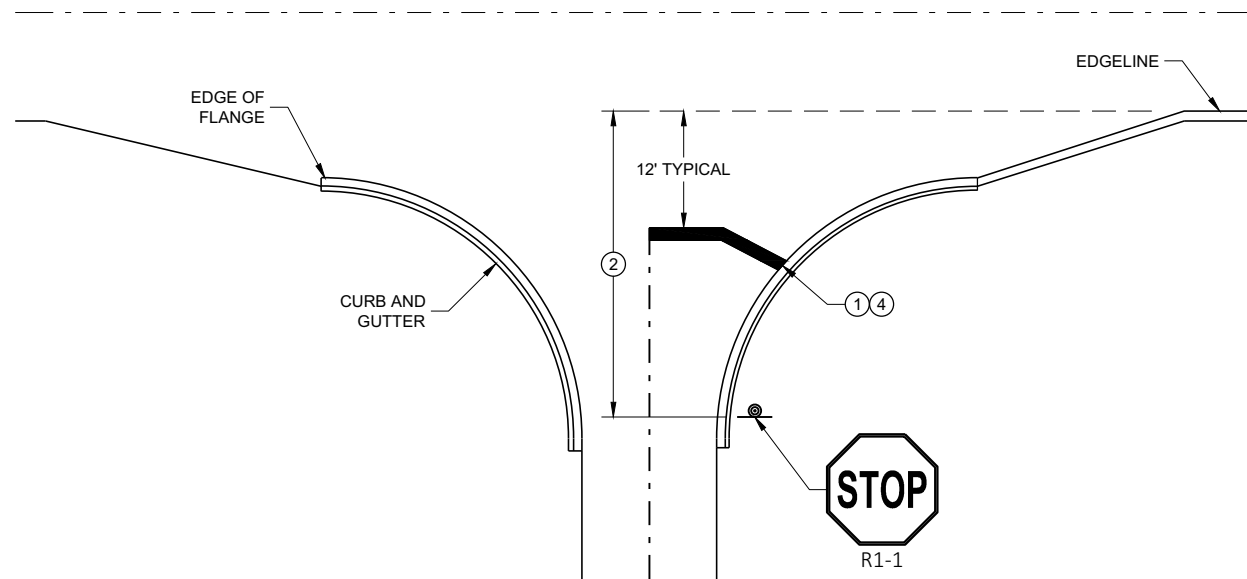
APPROVED
May 2023 DATE /S/ Jeannie Silver
STATE SIGNING AND MARKING ENGINEER

FHWA

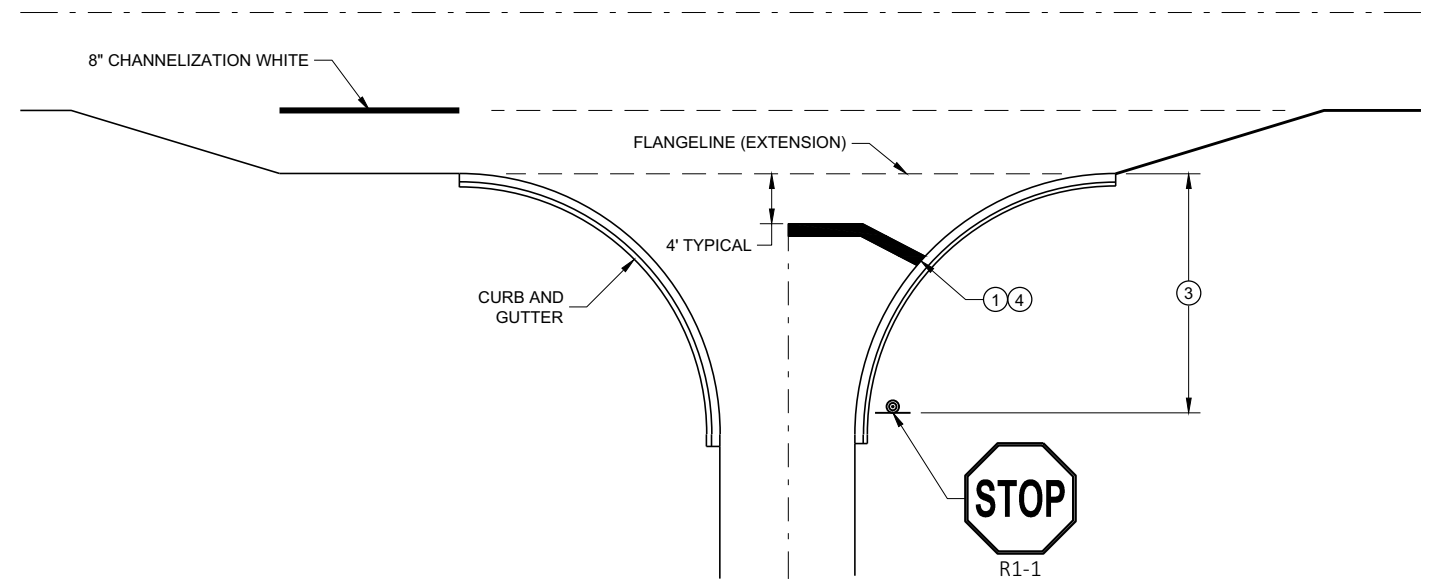
GENERAL NOTES

STOP SIGN SHALL BE PLACED A MINIMUM OF 6 FEET TO A MAXIMUM OF 50 FEET FROM THE EDGELINE LOCATION.

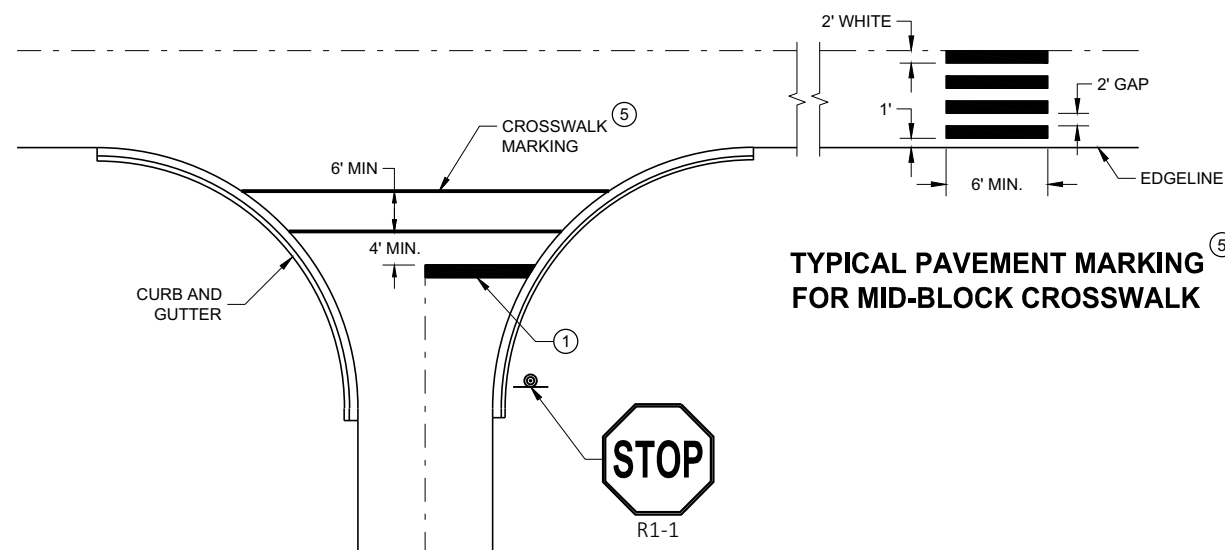
- ① 18-INCH STOP LINES MAY BE DELETED OR ADDED BY THE REGION MARKING ENGINEER BASED ON VISIBILITY AND SIGHT LINES.
- ② NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 40 FEET FROM THE EDGELINE.
- ③ NO STOP LINE IS REQUIRED IF STOP SIGN IS LESS THAN OR EQUAL TO 30 FEET FROM THE FLANGELINE EXTENSION.
- ④ MOVE CLOSER TO THE EDGE OF TRAVEL LINE AS NEEDED FOR VISIBILITY AND SIGHT LINES (NO CLOSER THAN 4 FEET).
- ⑤ LADDER BAR CROSSWALKS SHOULD ONLY BE USED FOR MID BLOCK CROSSINGS. USE 2 - 6" TRANSVERSE LINES INSTEAD.



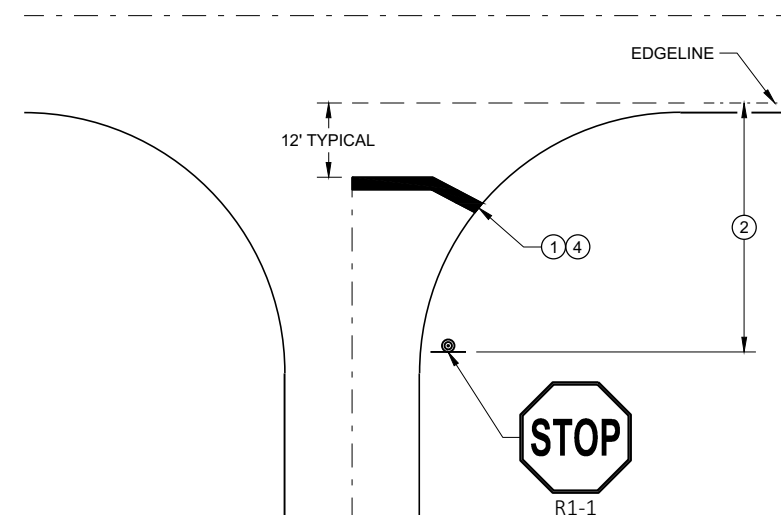
TYPICAL STOP LINE PAVEMENT MARKING WITH CURB AND GUTTER



TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH RIGHT TURN LANE



TYPICAL STOP LINE PAVEMENT MARKING FOR SIDEROADS WITH CROSSWALK MARKING

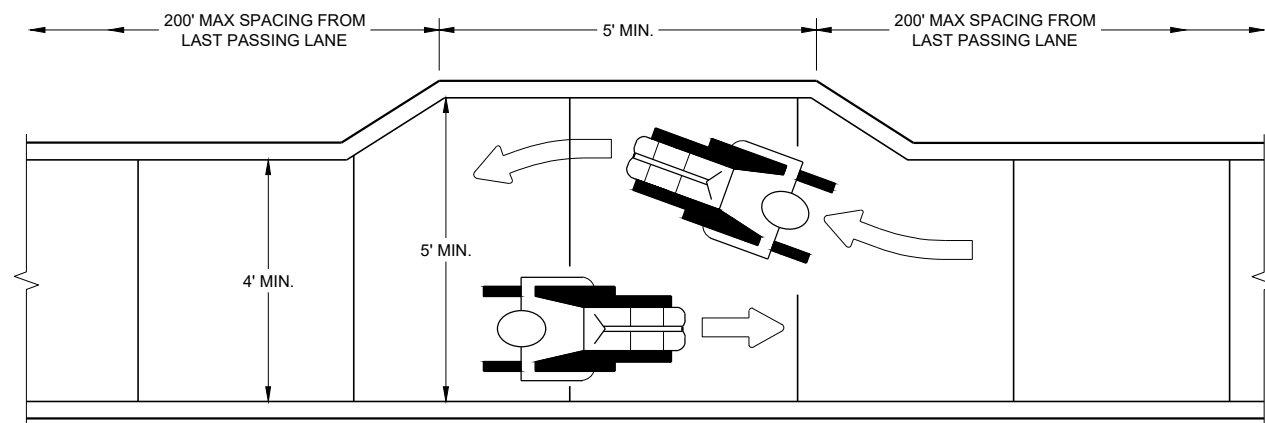


TYPICAL STOP LINE PAVEMENT MARKING WITHOUT CURB AND GUTTER

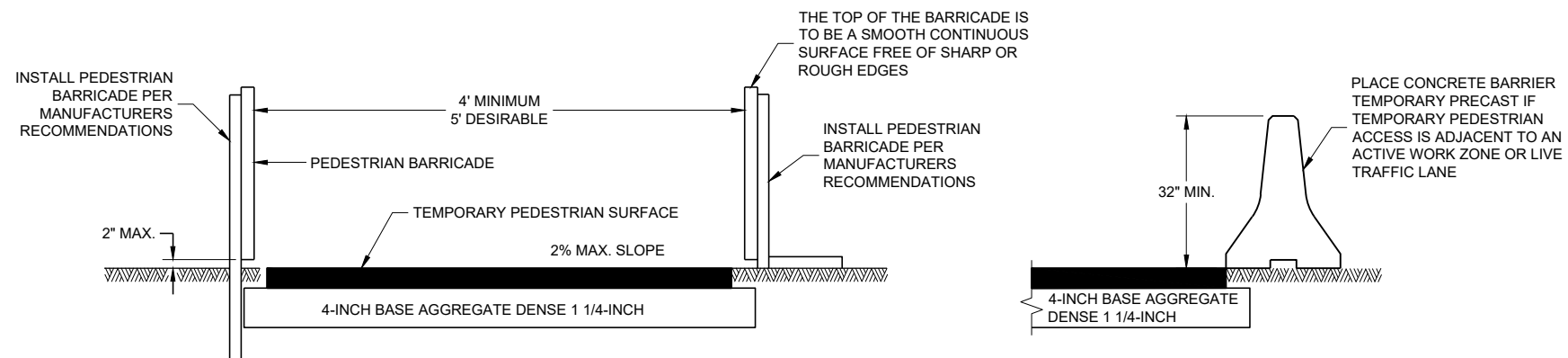
STOP LINE AND CROSSWALK PAVEMENT MARKING

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2019 /S/ Matthew Rauch
DATE STATE SIGNING AND MARKING ENGINEER
FHWA



NARROW SIDEWALK PASSING DETAIL



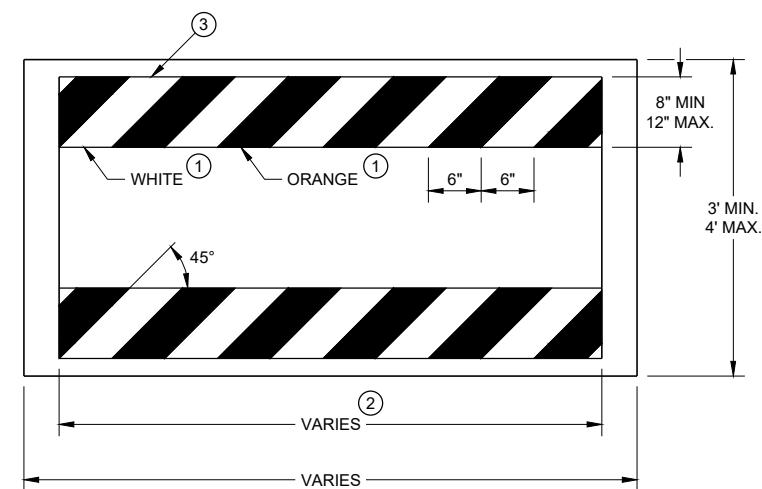
TEMPORARY PEDESTRIAN ACCESS

GENERAL NOTES

BARRICADE DEVICE SELECTED FROM APPROVED PRODUCT LIST

- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② SHEETING REQUIRED ON MORE THAN 50% OF BARRICADE WIDTH.
- ③ PLACE SHEETING ON BOTH SIDES OF THE BARRICADE.

* USE THIS DETAIL FOR SHEETING PLACEMENT REFERENCE.



TEMPORARY PEDESTRIAN BARRICADE*

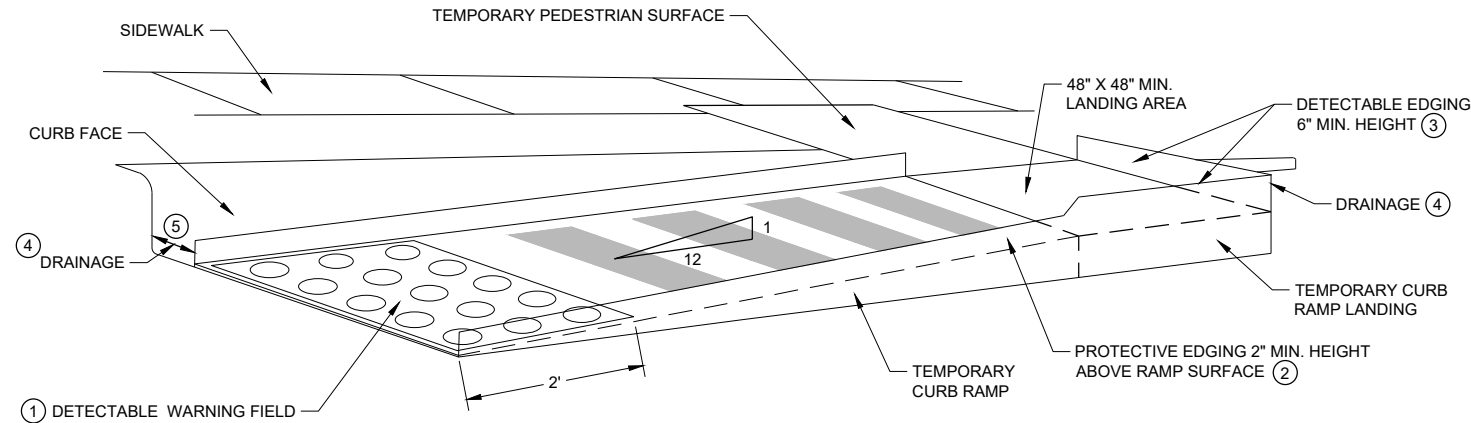
**TRAFFIC CONTROL,
PEDESTRIAN
ACCOMMODATION**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

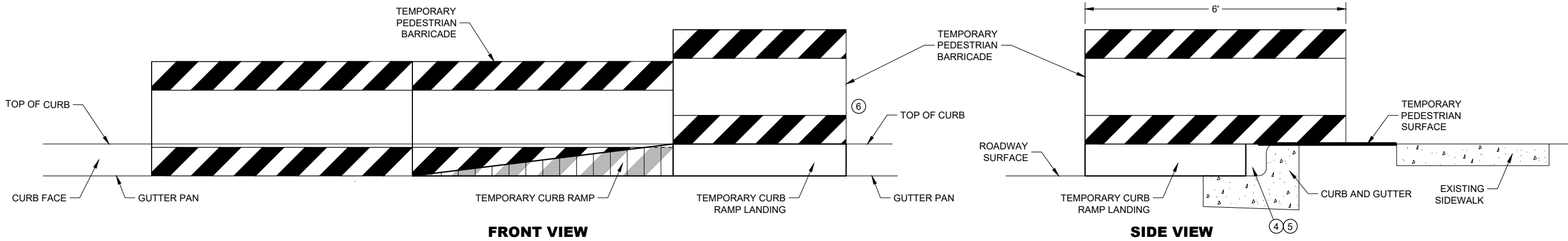
GENERAL NOTES

CURB RAMPS SHALL BE 48" MIN. WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE.
 CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.
 CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.
 LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.
 CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES MAY BE VERTICAL UP TO 1/4" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".

- ① INSTALL CONTRASTING TEMPORARY DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS, AS SHOWN IN THE PLANS.
- ② PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- ③ DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- ④ DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ⑤ ENSURE CURB RAMP IS OUT OF THE GUTTER PAN.
- ⑥ IF ONLY PART OF THE END PANEL OF TEMPORARY PEDESTRIAN BARRICADE PANEL IS NEEDED, EXTEND EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL HERE.



PERSPECTIVE VIEW

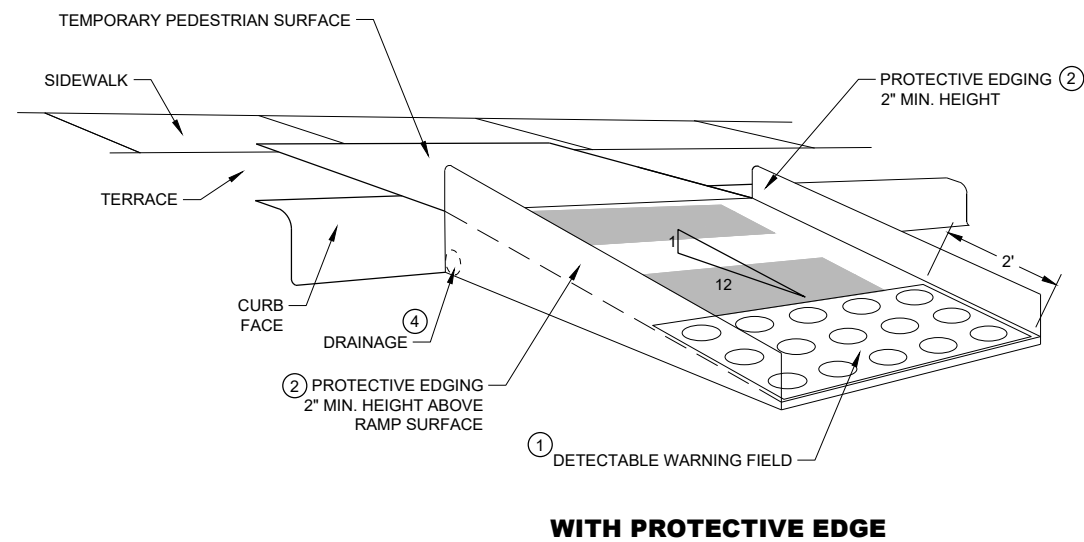
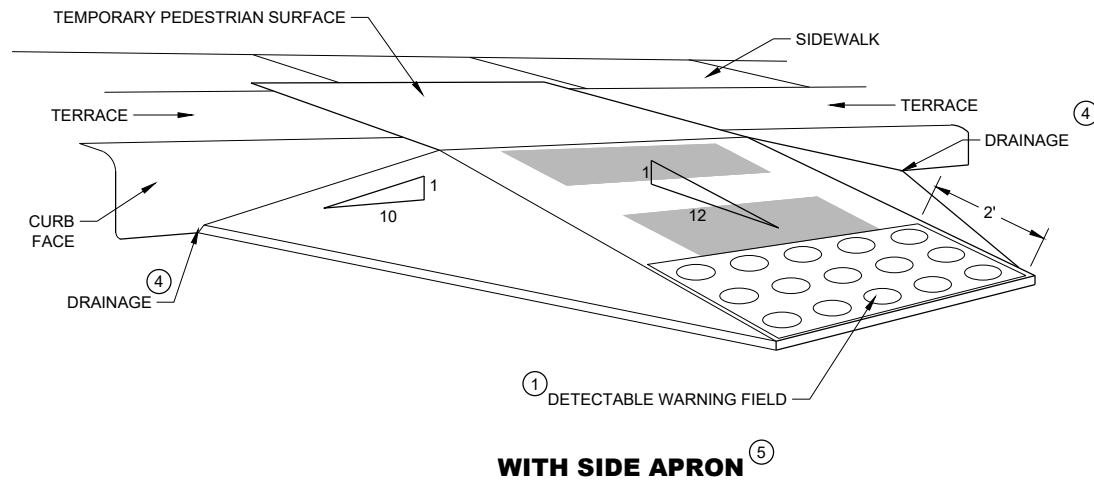


FRONT VIEW

SIDE VIEW

TEMPORARY CURB RAMP PARALLEL TO CURB

<p>TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION</p>
<p>STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION</p>



TEMPORARY CURB RAMP PERPENDICULAR TO CURB

GENERAL NOTES

CURB RAMPS SHALL BE 48" MINIMUM WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE.

ALTERNATE SIDEWALK WORK BETWEEN LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.

CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.

CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.

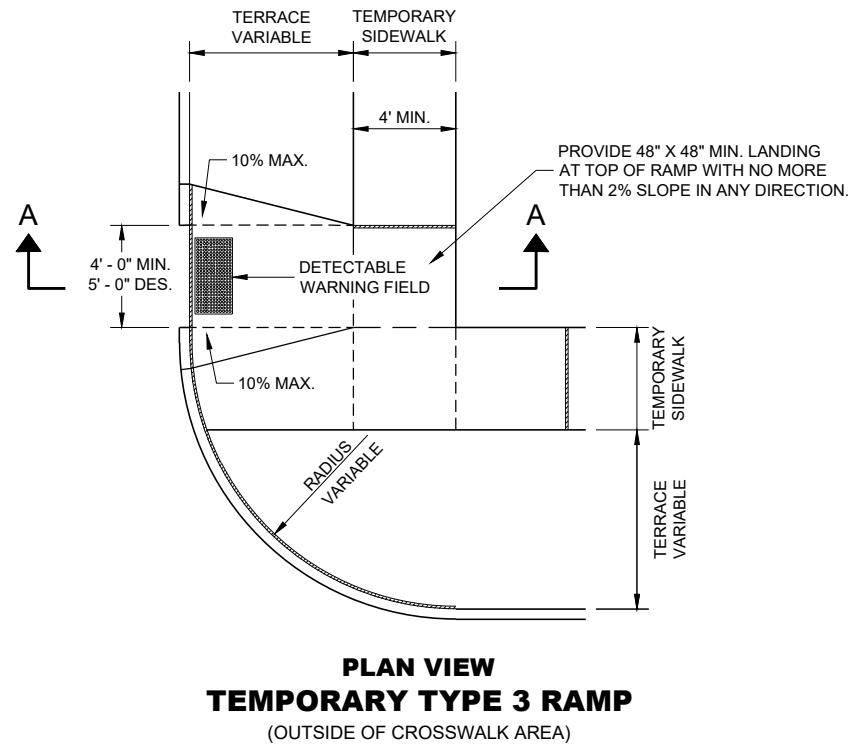
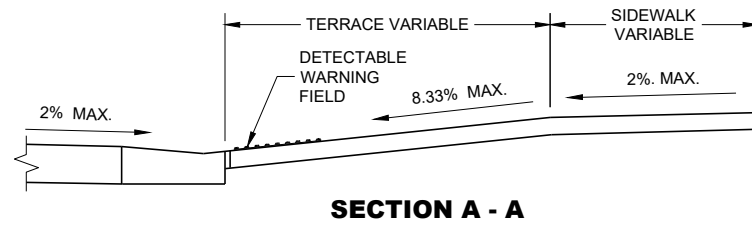
LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.

CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES MAY BE VERTICAL UP TO 1/4" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".

- ① INSTALL CONTRASTING TEMPORARY DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS, AS SHOWN IN THE PLANS
- ② PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- ③ DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- ④ DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ⑤ CAN ONLY BE USED FOR RAMPS WITH 6" OR LESS OF VERTICAL CHANGE.

GENERAL NOTES

- BARRICADE DEVICE SELECTED FROM APPROVED PRODUCT LIST
- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② SHEETING REQUIRED ON MORE THAN 50% OF BARRICADE WIDTH.
- ③ PLACE SHEETING ON BOTH SIDES OF THE BARRICADE.
- ★ USE THIS DETAIL FOR SHEETING PLACEMENT REFERENCE.



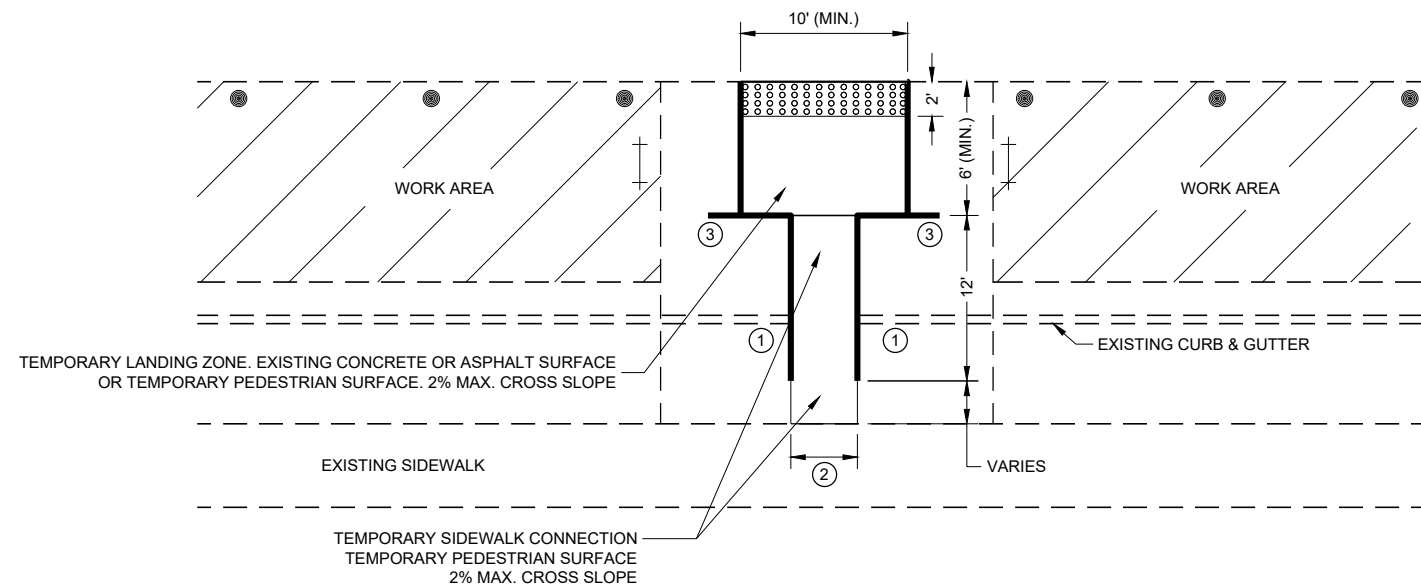
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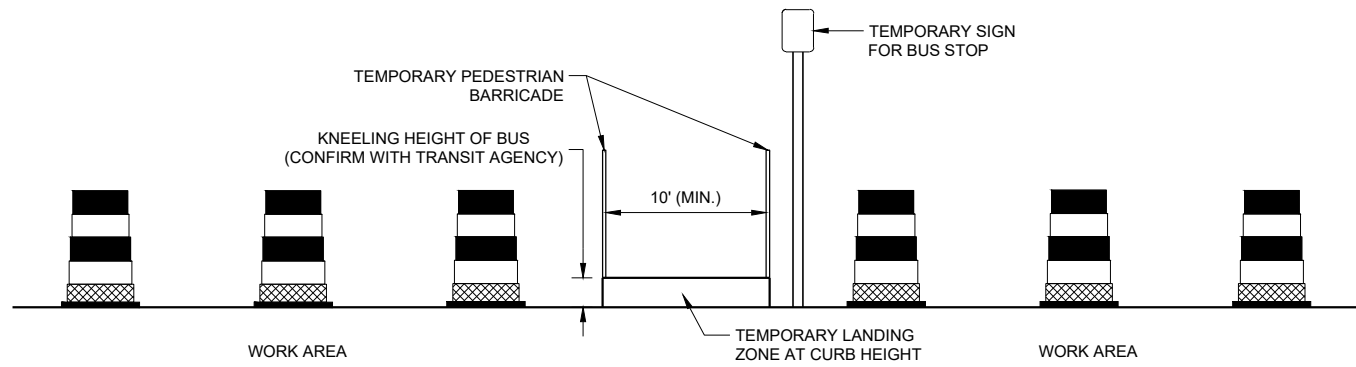
SDD 15D30-09d

SDD 15D30-09d

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2023 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	



PLAN VIEW



PROFILE VIEW
TEMPORARY BUS STOP PAD

GENERAL NOTES

- TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.
- NOTIFY THE BUS COMPANY 7 DAYS IN ADVANCE OF THE BUS STOP RELOCATION.
- PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMP OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.
- CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES MAY BE VERTICAL UP TO 1/4" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".
- CURB RAMP AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.

- ① DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ② 5' WIDE MIN. WITH TEMPORARY PEDESTRIAN BARRICADE, 10' WIDE MIN. WITHOUT TEMPORARY PEDESTRIAN BARRICADE.
- ③ PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE INTO THIS SPACE.







LEGEND

- TRAFFIC CONTROL DRUM
- ⊥ TYPE III BARRICADE
- TEMPORARY PEDESTRIAN BARRICADE
- ◻ TEMPORARY DETECTABLE WARNING FIELD
- ▨ WORK AREA

**TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION**

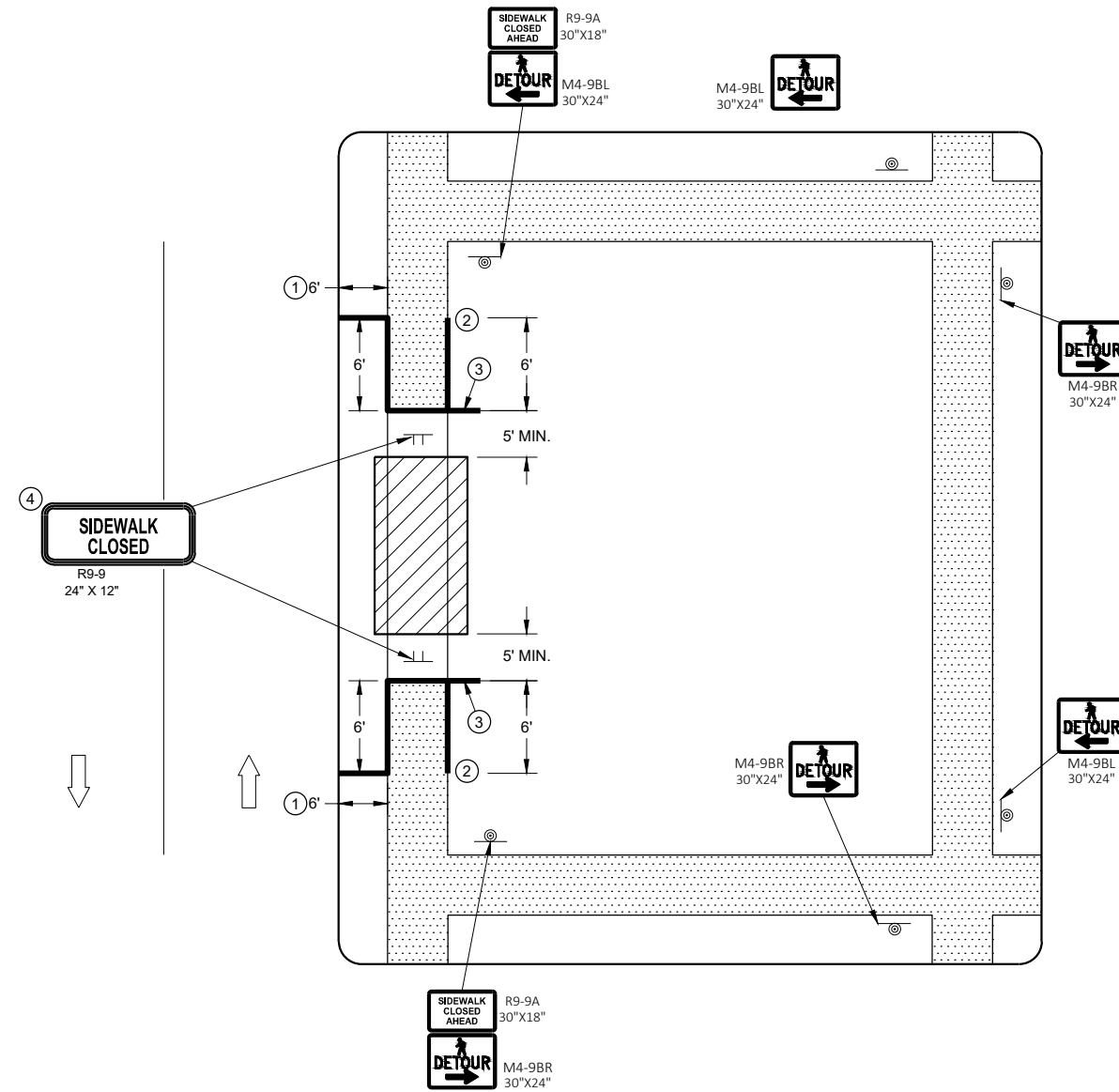
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  SIGN ON TEMPORARY SUPPORT
-  UNDER PEDESTRIAN TRAFFIC
-  WORK AREA
-  TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC

GENERAL NOTES








- SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.
- WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.
- SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.
- PLACE TEMPORARY PEDESTRIAN BARRICADE TO FIT FIELD CONDITIONS, AVOIDING CONFLICTS WITH DRIVEWAYS AND OTHER EXISTING FEATURES.
- ① IF TERRACE IS LESS THAN 6 FEET WIDE, OMIT TEMPORARY PEDESTRIAN BARRICADE FROM THE SIDEWALK TO THE CURB.
 - ② PLACE BARRICADE CLOSURE SO THAT THE TEMPORARY PEDESTRIAN BARRICADE END IS AT THE LAST OPEN SIDEWALK ACCESS TO RESIDENCES OR BUSINESSES BEFORE THE SIDEWALK CLOSURE.
 - ③ IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
 - ④ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.



SIDEWALK DETOUR, SIDEWALK ONLY ON ONE SIDE

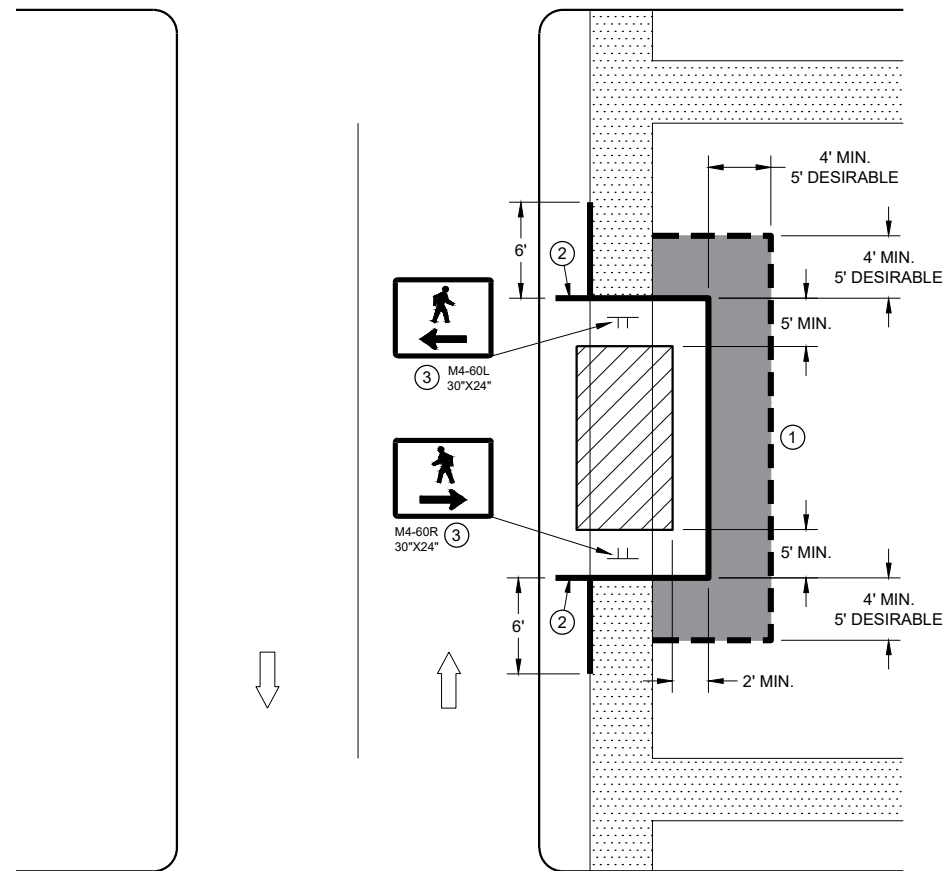
TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  WORK AREA
-  UNDER PEDESTRIAN TRAFFIC
-  TEMPORARY PEDESTRIAN SURFACE
-  TEMPORARY PEDESTRIAN BARRICADE
-  OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC










GENERAL NOTES

- TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.
- SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.
- WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.
- SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.
- ① USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
- ② IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
- ③ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.



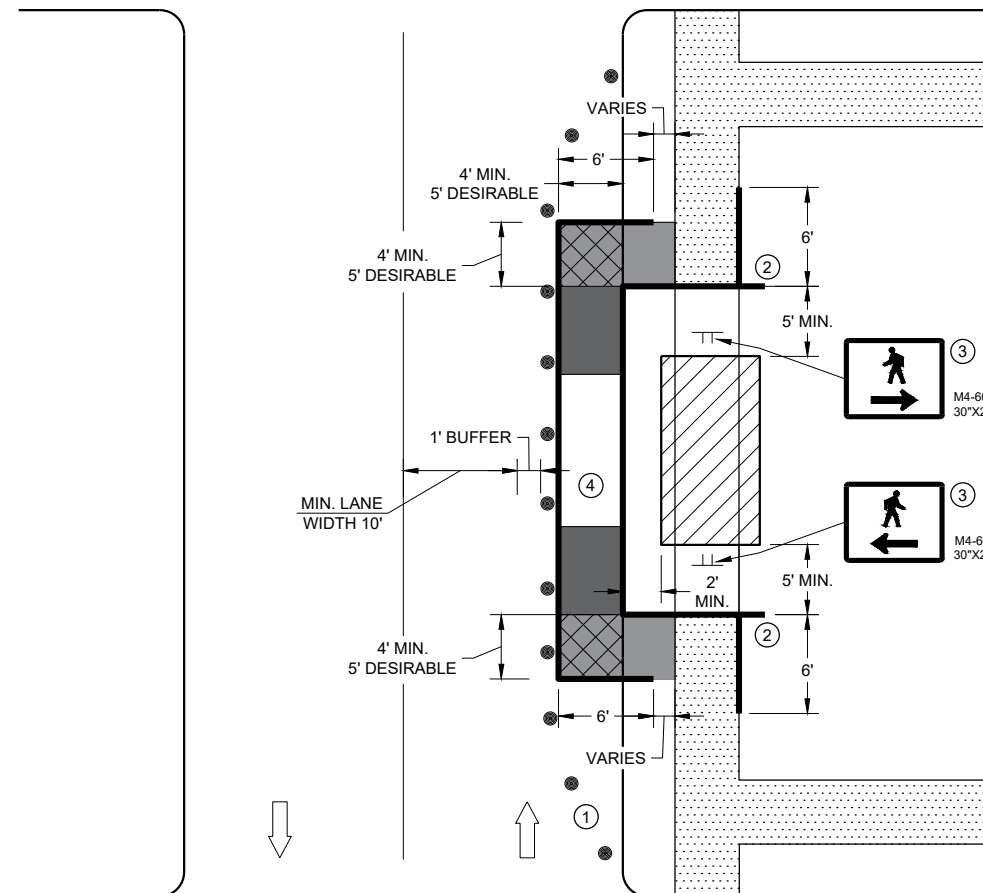
**SIDEWALK DIVERSION
SINGLE SIDE**

LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL DRUM
-  WORK AREA
-  UNDER PEDESTRIAN TRAFFIC
-  TEMPORARY CURB RAMP
-  TEMPORARY PEDESTRIAN SURFACE "A"
-  TEMPORARY PEDESTRIAN SURFACE "B"
-  TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC

GENERAL NOTES

- TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.
- SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.
- WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.
- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND BUFFER SPACE REQUIRED.
 - ② PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL PAST THE SIDEWALK ON THE SIDE AWAY FROM THE ROAD.
 - ③ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.
 - ④ USE EXISTING PAVEMENT SURFACE. IF EXISTING PAVEMENT SURFACE HAS BEEN REMOVED, USE A TEMPORARY PEDESTRIAN SURFACE.



SIDEWALK DIVERSION, SINGLE SIDE

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SDD 15D30 - 09h

SDD 15D30 - 09h

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

IF PEDESTRIAN PUSH BUTTONS ARE PRESENT ON THE EXISTING FACILITY, ENSURE THEY ARE MAINTAINED/ACCESSIBLE FOR PEDESTRIAN USE THROUGHOUT THE TEMPORARY PEDESTRIAN ACCOMMODATIONS.











SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

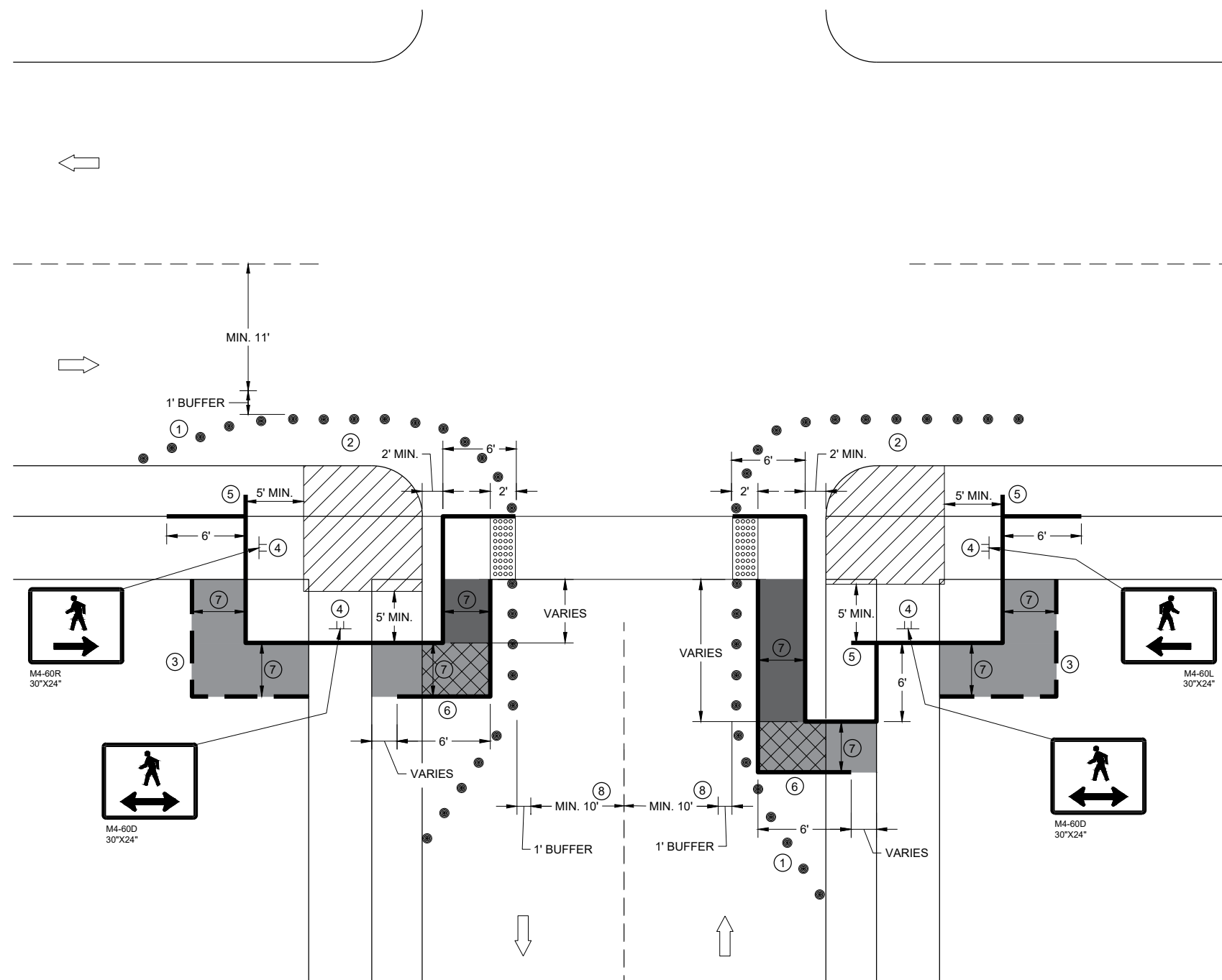
TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG

WHEN TEMPORARY PEDESTRIAN BARRICADE RUNS PARALLEL ALONG THE SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND PROPER BUFFER SPACE REQUIRED.
- ② PROVIDE ADEQUATE SPACE FOR CONTRACTOR OPERATIONS
- ③ USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
- ④ MOUNTING HEIGHT OF 5 FEET FROM SIDEWALK SURFACE TO BOTTOM OF SIGN.
- ⑤ PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL IN THE SIDEWALK TERRACE.
- ⑥ IF TEMPORARY PEDESTRIAN BARRICADE DOES NOT REACH THE FACE OF THE CURB, USE AN ADDITIONAL PANEL AND EXTEND INTO THE TERRACE.
- ⑦ 4 FEET MINIMUM, 5 FEET DESIRABLE
- ⑧ IF MINIMUM LANE WIDTHS CAN'T BE ATTAINED, CURB RAMPS MAY NEED TO BE CONSTRUCTED AT SEPARATE TIMES.

LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL DRUM
-  WORK AREA
-  TEMPORARY CURB RAMP
-  TEMPORARY PEDESTRIAN SURFACE "A"
-  TEMPORARY PEDESTRIAN SURFACE "B"
-  TEMPORARY DETECTABLE WARNING FIELD
-  TEMPORARY PEDESTRIAN BARRICADE
-  OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC



**CURB RAMP PEDESTRIAN TRAFFIC CONTROL
SIDEWALK ON SINGLE SIDE**

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

GENERAL NOTES

IF PEDESTRIAN PUSH BUTTONS ARE PRESENT ON THE EXISTING FACILITY, ENSURE THEY ARE MAINTAINED/ACCESSIBLE FOR PEDESTRIAN USE THROUGHOUT THE TEMPORARY PEDESTRIAN ACCOMMODATIONS.











SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.

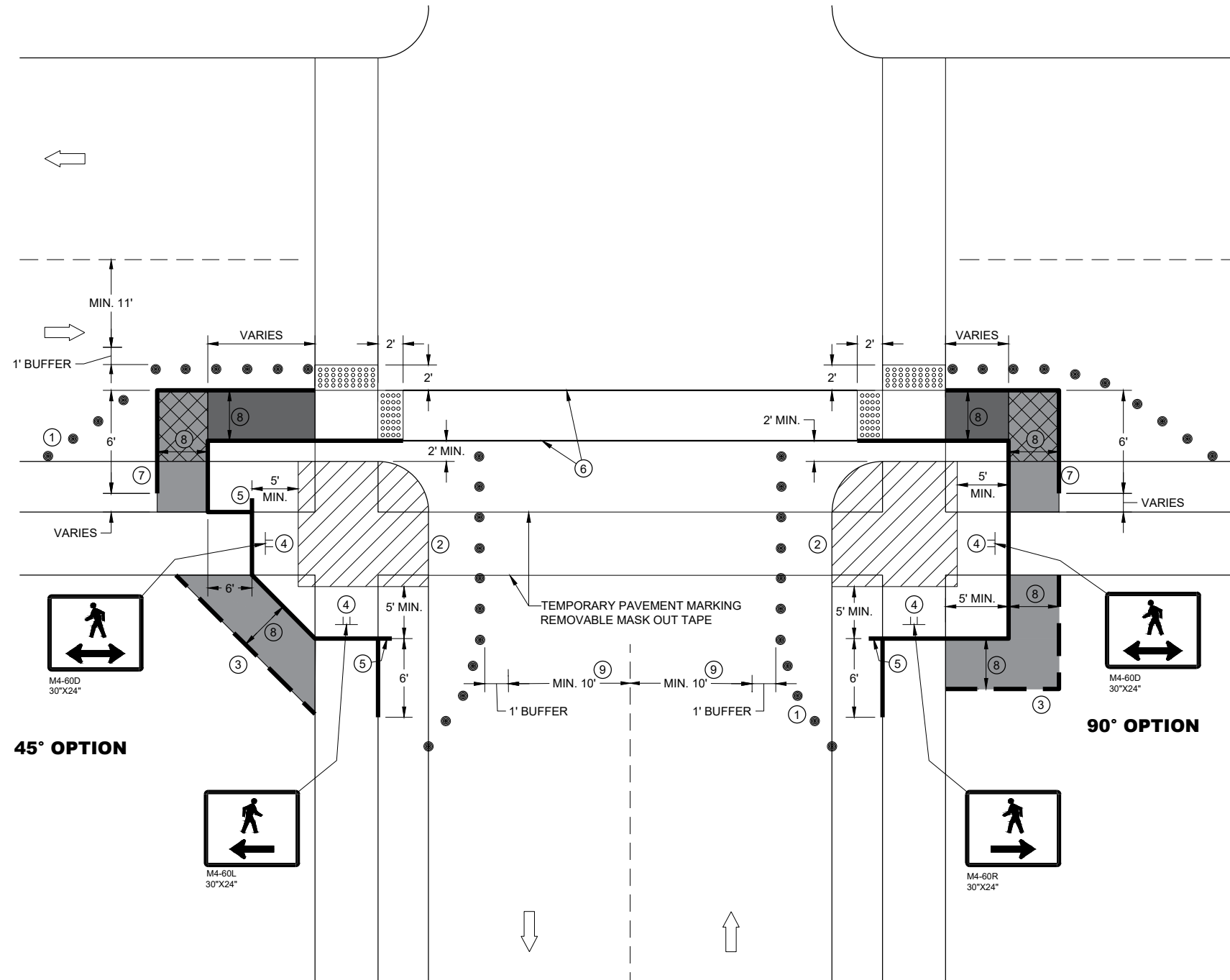
TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG

WHEN TEMPORARY PEDESTRIAN BARRICADE RUNS PARALLEL ALONG THE SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.

- ① SHOULDER OR LANE CLOSURE ADVANCE WARNING AND PROPER BUFFER SPACE REQUIRED.
- ② PROVIDE ADEQUATE SPACE FOR CONTRACTOR OPERATIONS
- ③ USE TEMPORARY PEDESTRIAN BARRICADE TO SEPARATE PEDESTRIANS FROM DROP OFFS OR FOR ADDITIONAL PEDESTRIAN CHANNELIZATION.
- ④ MOUNTING HEIGHT OF 5 FEET FROM SIDEWALK SURFACE TO BOTTOM OF SIGN.
- ⑤ PLACE EXCESS PORTION OF TEMPORARY PEDESTRIAN BARRICADE PANEL IN THE SIDEWALK TERRACE.
- ⑥ WHITE 6" TEMPORARY PAVEMENT MARKING
- ⑦ IF TEMPORARY PEDESTRIAN BARRICADE DOES NOT REACH THE FACE OF THE CURB, USE AN ADDITIONAL PANEL AND EXTEND INTO THE TERRACE.
- ⑧ 4 FEET MINIMUM, 5 FEET DESIRABLE
- ⑨ IF MINIMUM LANE WIDTHS CAN'T BE ATTAINED, CURB RAMPS MAY NEED TO BE CONSTRUCTED AT SEPARATE TIMES.

LEGEND

-  SIGN ON TEMPORARY SUPPORT
-  TRAFFIC CONTROL DRUM
-  WORK AREA
-  TEMPORARY CURB RAMP
-  TEMPORARY PEDESTRIAN SURFACE "A"
-  TEMPORARY PEDESTRIAN SURFACE "B"
-  TEMPORARY DETECTABLE WARNING FIELD
-  TEMPORARY PEDESTRIAN BARRICADE
-  OPTIONAL TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC









CURB RAMP PEDESTRIAN TRAFFIC CONTROL

**TRAFFIC CONTROL,
PEDESTRIAN ACCOMMODATION**

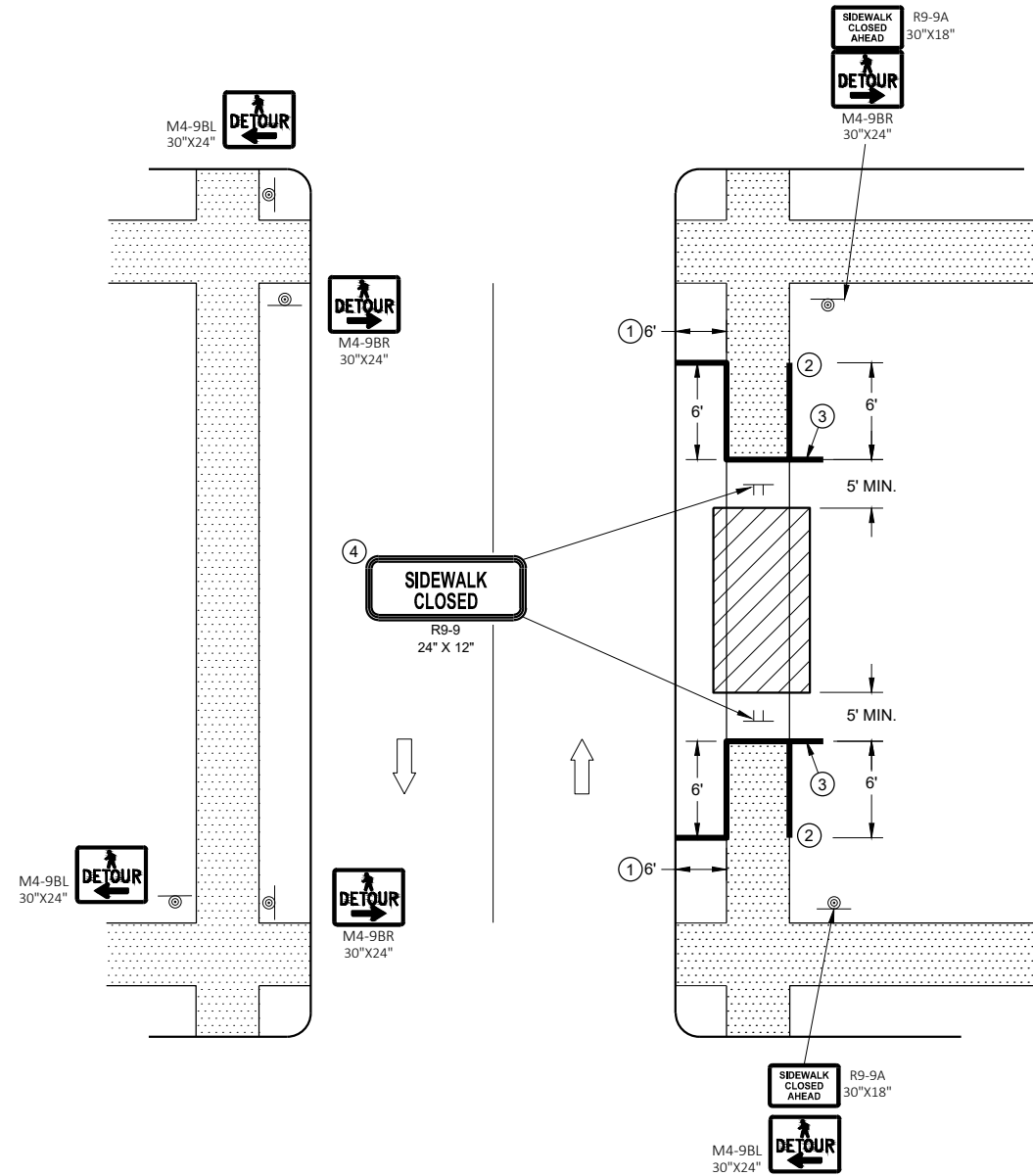
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  SIGN ON TEMPORARY SUPPORT
-  UNDER PEDESTRIAN TRAFFIC
-  WORK AREA
-  TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC

GENERAL NOTES

- SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.
- WHERE TEMPORARY BARRICADE RUNS PARALLEL ALONG SIDEWALK, PLACE THE FACE OF THE BARRICADE AT THE EDGE OF THE SIDEWALK.
- SIGNS THAT REMAIN IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.
- PLACE TEMPORARY PEDESTRIAN BARRICADE TO FIT FIELD CONDITIONS, AVOIDING CONFLICT WITH DRIVEWAYS AND OTHER EXISTING FEATURES.
- ① IF TERRACE IS LESS THAN 6 FEET WIDE, OMIT TEMPORARY PEDESTRIAN BARRICADE FROM THE SIDEWALK TO THE CURB.
 - ② PLACE BARRICADE CLOSURE SO THAT THE TEMPORARY PEDESTRIAN BARRICADE END IS AT THE LAST OPEN SIDEWALK ACCESS TO RESIDENCES OR BUSINESSES BEFORE THE SIDEWALK CLOSURE.
 - ③ IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
 - ④ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF SIGN.



SIDEWALK DETOUR, SIDEWALK ON BOTH SIDES








TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

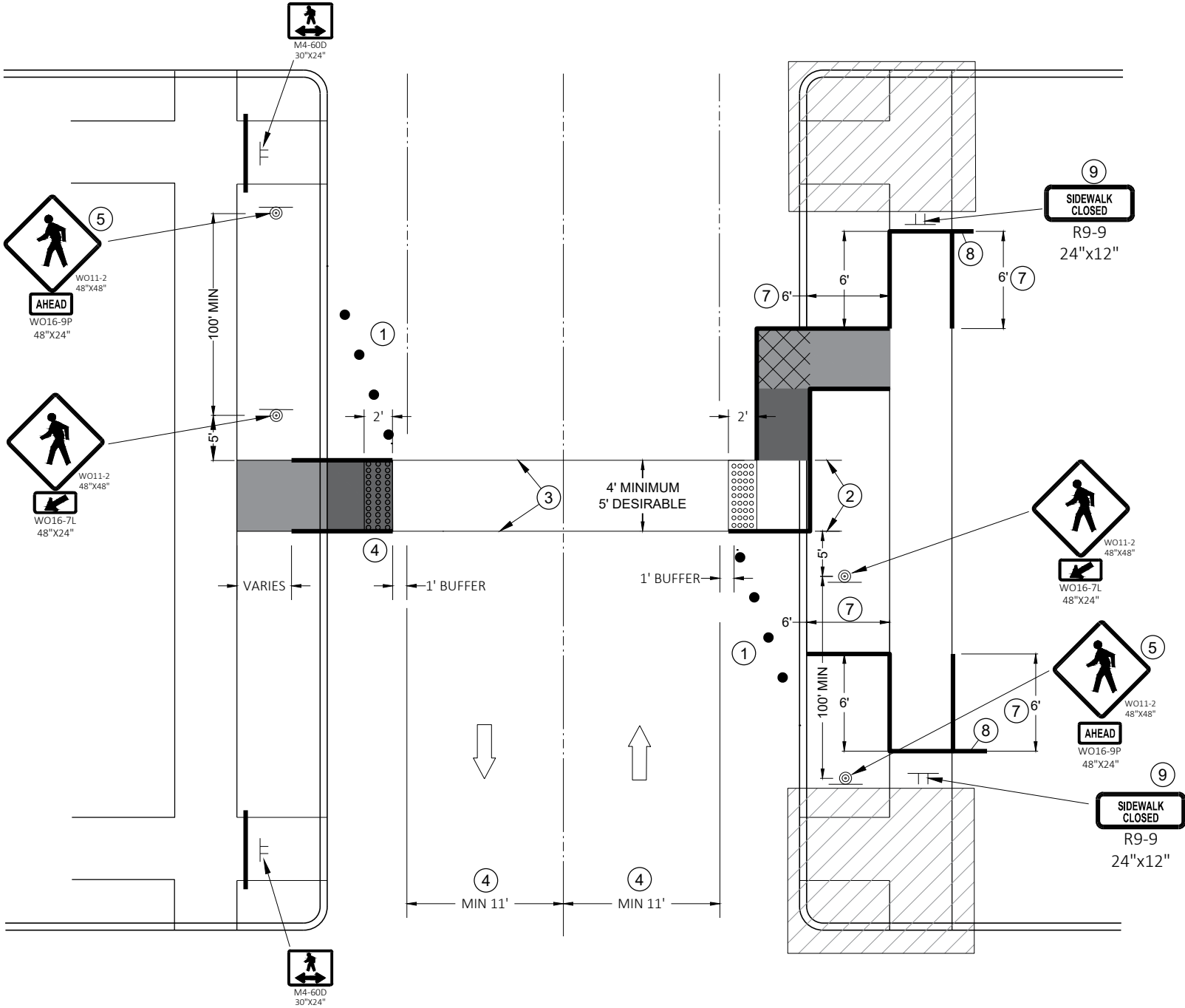
GENERAL NOTES

TYPICAL TEMPORARY PEDESTRIAN BARRICADE PANEL IS 6 FEET LONG.
 SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE WISCONSIN STANDARD SIGN PLATES.
 SEE OTHER PEDESTRIAN ACCOMMODATION DETAILS FOR SIGNING AND DEVICES FOR DIFFERENT PEDESTRIAN FACILITIES CLOSURES.

- ① SHOULDER OR LANE CLOSURE ADVANCED WARNING AND PROPER BUFFER SPACE REQUIRED.
- ② 4 FEET MINIMUM, 5 FEET DESIRABLE.
- ③ WHITE 6" TEMPORARY PAVEMENT MARKING.
- ④ IF MINIMUM LANE WIDTHS CAN'T BE ATTAINED, PERPENDICULAR CURB RAMP MAY NEED TO BE UTILIZED.
- ⑤ IF MINIMUM 100' SPACING FROM THE MID-BLOCK CROSSING CANNOT BE ATTAINED BEFORE THE INTERSECTION, REMOVE THIS SIGN ASSEMBLY.
- ⑥ IF TERRACE IS LESS THAN 6 FEET WIDE, OMIT TEMPORARY PEDESTRIAN BARRICADE FROM THE SIDEWALK TO THE CURB.
- ⑦ PLACE BARRICADE CLOSURE SO THAT THE TEMPORARY PEDESTRIAN BARRICADE END IS AT THE LAST OPEN SIDEWALK ACCESS TO RESIDENCES OR BUSINESSES BEFORE THE SIDEWALK CLOSURE.
- ⑧ IF TEMPORARY PEDESTRIAN BARRICADE PANEL IS WIDER THAN THE SIDEWALK WIDTH, THE PORTION OF THE EXCESS PANEL SHOULD EXTEND INTO THE TERRACE.
- ⑨ MOUNTING HEIGHT OF 5 FEET FROM THE SURFACE TO THE BOTTOM OF THE SIGN.

LEGEND

-  TRAFFIC CONTROL DRUM
-  SIGN ON TEMPORARY SUPPORT
-  TEMPORARY CURB RAMP
-  TEMPORARY DETECTABLE WARNING FIELD
-  TEMPORARY PEDESTRIAN SURFACE "A"
-  TEMPORARY PEDESTRIAN SURFACE "B"
-  WORK AREA
-  TEMPORARY PEDESTRIAN BARRICADE
-  DIRECTION OF TRAFFIC



TEMPORARY PEDESTRIAN CROSSING

TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

Notes



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