

# STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

## PLAN OF PROPOSED IMPROVEMENT

# NORTHWEST REGION, USH 53 BRIDGE DECK SEALING

BRIDGE DECK SEALING (VARIOUS)

USH 53

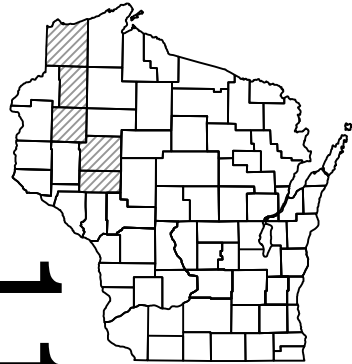
DOUGLAS, WASHBURN, BARRON, CHIPPEWA, EAU CLAIRE COUNTIES

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
1190-01-85		

ORDER OF SHEETS

Section No.	Title
1	Title
2	Typical Sections and Details
3	Estimate of Quantities
3	Miscellaneous Quantities
<del>4</del>	<del>Right of Way Plat</del>
<del>5</del>	<del>Plan and Profile</del>
6	Standard Detail Drawings
7	Sign Plates
8	Structure Plans
9	Computer Earthwork Data
9	Cross Sections

TOTAL SHEETS = 20



STATE PROJECT NUMBER
1190-01-85

END PROJECT  
B-16-112  
UN USH 53 OVER HILL AVE.  
DOUGLAS COUNTY  
X = 46° 43' 40"  
Y = 92° 04' 36"

BRIDGE DECK AND BRIDGE COLUMN  
SEALING. VARIOUS BRIDGES,  
SEE PLAN SHEETS FOR DETAILS

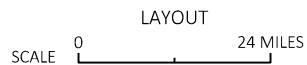
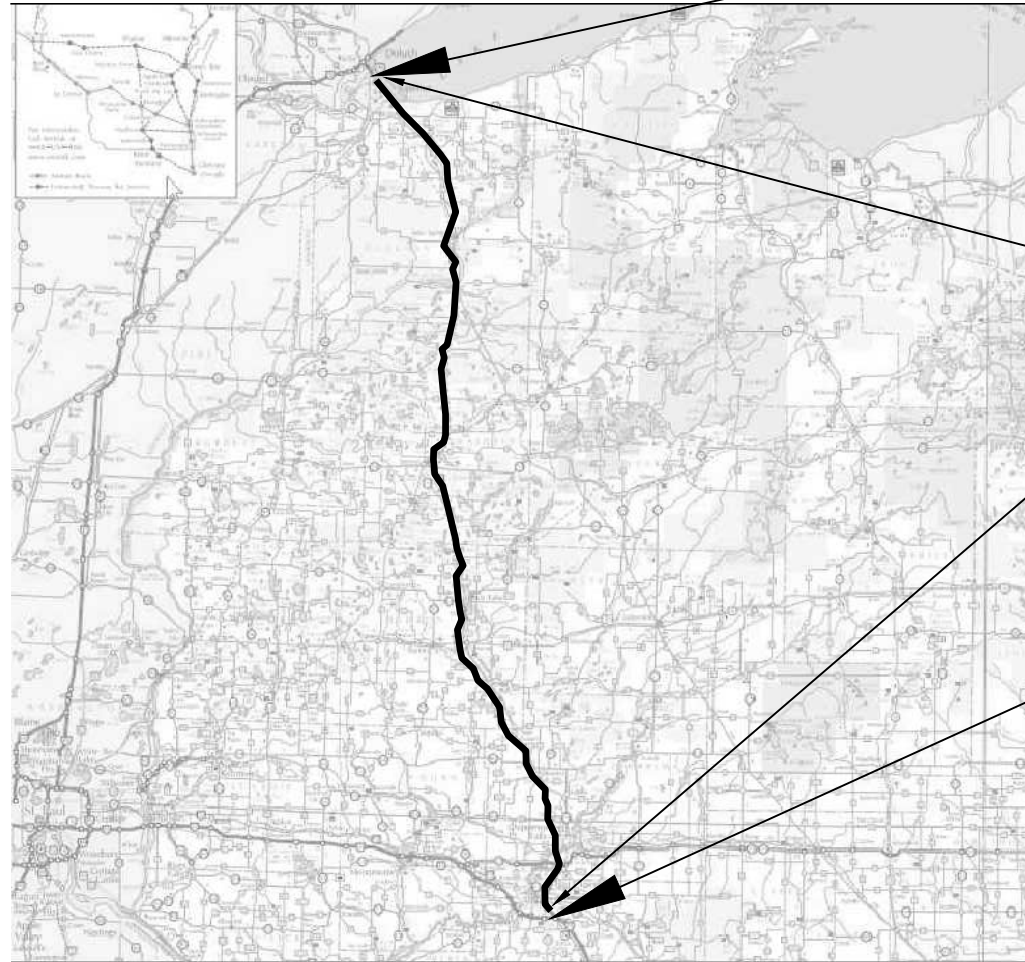
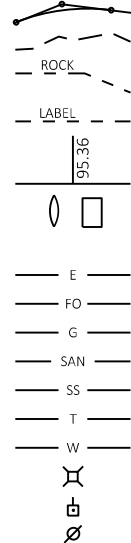
BEGIN PROJECT  
B-18-099  
ON CHT AA (GOLF ROAD)  
EAU CLAIRE COUNTY  
X = 44° 46' 38"  
Y = 91° 26' 03"

DESIGN DESIGNATION 1190-01-15

A.A.D.T.	LOW	=	3200
A.A.D.T.	HIGH	=	17900
D.H.V.		=	
D.D.		=	
T.		=	
DESIGN SPEED		=	70 MPH
ESALS		=	

CONVENTIONAL SYMBOLS

PLAN	PROFILE
CORPORATE LIMITS	GRADE LINE
PROPERTY LINE	ORIGINAL GROUND
LOT LINE	MARSH OR ROCK PROFILE (To be noted as such)
LIMITED HIGHWAY EASEMENT	SPECIAL DITCH
EXISTING RIGHT OF WAY	GRADE ELEVATION
PROPOSED OR NEW R/W LINE	CULVERT (Profile View)
SLOPE INTERCEPT	UTILITIES
REFERENCE LINE	ELECTRIC
EXISTING CULVERT	FIBER OPTIC
PROPOSED CULVERT (Box or Pipe)	GAS
COMBUSTIBLE FLUIDS	SANITARY SEWER
MARSH AREA	STORM SEWER
WOODED OR SHRUB AREA	TELEPHONE
	WATER
	UTILITY PEDESTAL
	POWER POLE
	TELEPHONE POLE



TOTAL NET LENGTH OF CENTERLINE = 3.501

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE LAT/LONG  
WGS 84 IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID  
COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID  
DISTANCES ARE THE SAME AS GROUND DISTANCES.

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
PREPARED BY	TOM ARMSTRONG
Surveyor	MICHAEL PEARSON
Designer	PHIL KEPPERS
Project Manager	MITCH FINNEGAN
Regional Examiner	JEFF OLSON
Regional Supervisor	
APPROVED FOR THE DEPARTMENT	<i>Philip Keppers</i>
DATE: 10/12/2023	(Signature)

PROJECT: 1190-01-85

UTILITIES: THERE ARE UTILITY FACILITIES WITHIN THE PROJECT AREA THAT ARE NOT SHOWN ON THE PLANS. COORDINATE CONSTRUCTION ACTIVITIES WITH A CALL TO DIGGERS HOTLINE AND/OR A DIRECT CALL TO THE UTILITIES THAT HAVE FACILITIES IN THE AREA. NOT ALL UTILITIES ARE MEMBERS OF DIGGERS HOTLINE.

DESIGN PROJECT MANAGER

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80 SUPERIOR, WI 548  
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DESIGN PROJECT LEADER

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AREA MAINTENANCE CONTACT

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AREA MAINTENANCE CONTACT

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GENERAL NOTES:

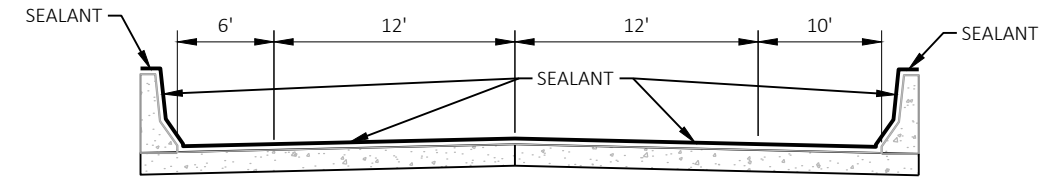
1. WORK CONSISTS OF PREPARATION OF THE UPPER SIDE OF THE CONCRETE DECK, PARAPET SURFACE AND PIER COLUMNS, AND APPLICATION OF CONCRETE PROTECTIVE SURFACE TREATMENT IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS AND RECOMMENDATIONS.
2. THERE ARE UTILITY FACILITIES WITHIN TH EPROJECT AREA THAT ARE NOT SHOWN ON THE PLANS. THE CONTRACTOR WILL COORDINATE CONSTRUCTION ACTIVITIES WITH A CALL TO DIGGERS HOTLINE AND/OR A DIRECT CALL TO THE UTILITIES THAT HAVE FACILITIES IN THE AREA. NOT ALL UTILITIES ARE MEMBERS OF DIGGERS HOTLINE.
3. USH 53 WILL BE KEPT OPEN TO A MINIMUM OF ONE LANE OF TRAFFIC IN EACH DIRECTION AT ALL TIMES.
4. LANE CLOSURES WILL ONLY BE ALLOWED DURING TIMES SPECIFIED IN THE SPECIAL PROVISIONS.
5. SIGNS AND DEVICES WILL BE IN CONFORMANCE WITH THE LATEST MANUAL OF TRAFFIC CONTROL DEVICES (MUTCD).
6. THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES WILL BE ADJUSTED TO FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

RAILROAD PUBLIC PROJECTS OFFICER  
MATTHEW TURNER  
1625 DEPOT STREET  
STEVENS POINT, WI 54481  
715-345-2503

SPECIAL PROJECTS MANAGER-  
INDUSTRY & PUBLIC PROJECTS  
CHRIS T. KECKEISEN  
1400 DOUGLAS, MS 0910  
OMAHA, NE 68179  
402-544-5131

RAILROAD NOTES:

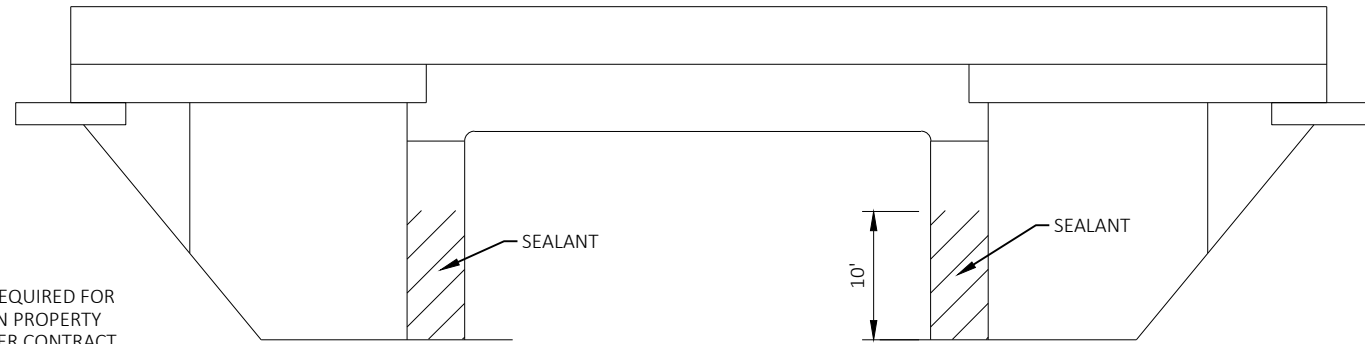
RAILROAD INSURANCE AND COORDINATION IS REQUIRED FOR ANY AND ALL WORK ON STRUCTURES THAT SPAN PROPERTY OWNED BY RAILROAD COMPANIES. WORK UNDER CONTRACT INVOLVES WORK ON THE UPPER SIDE OF THE STRUCTURES THAT SPAN PROPERTY OWNED BY THE FOLLOWING RAILROAD COMPANIES. SEE SPECIAL PROVISIONS FOR CONTACT INFORMATION.  
-UNION PACIFIC RAILROAD  
-WISCONSIN CENTRAL LTD



DECK CROSS SECTION

PARAPET NOTES:

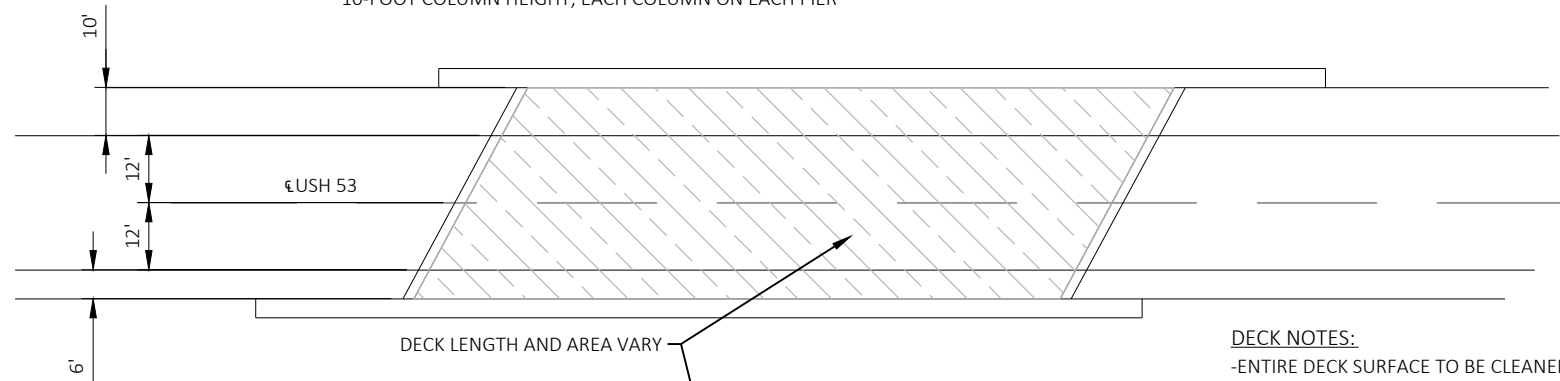
- INSIDE FACE AND TOP TO BE CLEANED AND SEALED
- TOP SURFACE TO BE ROLLED OR BRUSHED
- PARAPET TYPE AND HEIGHT VARIES
- SURFACE AREA INCLUDED IN QUANTITY SUMMARY
- AVERAGE 2' 6" HEIGHT USED FOR QUANTITY ESTIMATE



BRIDGE PROFILE VIEW

PIER COLUMN NOTES:

-PIER COLUMN SEALING ESTIMATED QUANTITY IS BASED ON A 10-FOOT COLUMN HEIGHT, EACH COLUMN ON EACH PIER



DECK LENGTH AND AREA VARY

DECK NOTES:

- ENTIRE DECK SURFACE TO BE CLEANED AND SEALED
- INLET PROTECTION REQUIRED DURING CLEANING AND SEALING WORK ON THE STRUCTURES WITH FLOOR DRAINS
- USE TYPE FF FABRIC DURING CLEANING ACTIVITIES PAID UNDER ITEM 628.7010
- USE IMPERVIOUS FABRIC DURING SEALING ACTIVITIES PAID UNDER ITEM 628.7010

SEALANT APPLICATION AREA

BRIDGE DECKS PLAN VIEW  
USH 53 TYPICAL 4 LANE DIVIDED



Dial 811 or (800)242-8511

www.DiggersHotline.com

Estimate Of Quantities

1190-01-85

Line	Item	Item Description	Unit	Total	Qty
0002	213.0100	Finishing Roadway (project) 01. 1190-01-85	EACH	1.000	1.000
0004	502.3215	Protective Surface Treatment Reseal	SY	31,042.000	31,042.000
0006	619.1000	Mobilization	EACH	1.000	1.000
0008	628.7010	Inlet Protection Type B	EACH	10.000	10.000
0010	643.0300	Traffic Control Drums	DAY	192.000	192.000
0012	643.0420	Traffic Control Barricades Type III	DAY	13.000	13.000
0014	643.0705	Traffic Control Warning Lights Type A	DAY	26.000	26.000
0016	643.0715	Traffic Control Warning Lights Type C	DAY	89.000	89.000
0018	643.0800	Traffic Control Arrow Boards	DAY	4.000	4.000
0020	643.0900	Traffic Control Signs	DAY	20.000	20.000
0022	643.1051	Traffic Control Signs PCMS with Cellular Communications	DAY	16.000	16.000
0024	643.5000	Traffic Control	EACH	1.000	1.000

3

3

PROTECTIVE SURFACE TREATMENT RESEAL						
						502.3215
						PROTECTIVE SURFACE TREATMENT RESEAL
CAT	STRUCTURE ID	VILLAGE, TOWN OR CITY	FEATURE ON	FEATURE UNDER	SURFACE TYPE	SY
<b>BARRON COUNTY</b>						
0010	B-030-195	T-OAK GROVE	USH 53 SB	CTH V	Bridge Deck	725
0010	B-03-0196	T-OAK GROVE	USH 53 NB	CTH V	Bridge Deck	729
0010	B-03-0027	T-PRAIRIE LAKE	CTH D	USH 53	Bridge Column	32
0010	B-03-0028	T-PRAIRIE LAKE	CTH OO	USH 53	Bridge Column	32
0010	B-03-0029	T-PRAIRIE LAKE	21ST ST	USH 53	Bridge Column	32
0010	B-03-0041	T-STANLEY	15TH AVE	USH 53	Bridge Column	21
0010	B-03-0046	C-RICE LAKE	ALLEN ST	USH 53	Bridge Column	42
0010	B-03-0047	C-RICE LAKE	AUGUSTA ST	USH 53	Bridge Column	32
0010	B-03-0054	T-STANLEY	16 1/2 AVE	USH 53	Bridge Column	32
0010	B-03-0055	T-OAK GROVE	CTH B	USH 53	Bridge Column	21
<b>BARRON COUNTY TOTAL</b>						1,698

PROTECTIVE SURFACE TREATMENT RESEAL						
						502.3215
						PROTECTIVE SURFACE TREATMENT RESEAL
CAT	STRUCTURE ID	VILLAGE, TOWN OR CITY	FEATURE ON	FEATURE UNDER	SURFACE TYPE	SY
<b>CHIPPEWA COUNTY</b>						
0010	B-09-0056	T-TILDEN	USH 53	S	Bridge Deck	864
0010	B-09-0057	T-TILDEN	USH 53 NB	S	Bridge Deck	864
0010	B-09-0089	T-BLOOMER	USH 53 NB	72ND ST.	Bridge Deck	858
0010	B-09-0090	T-BLOOMER	USH 53 SB	72ND ST.	Bridge Deck	878
0010	B-09-0228	T-HALLIE	USH 53SB	UP RR & BIKE PATH	Bridge Deck	1130
0010	B-09-0229	T-HALLIE	USH 53NB	UP RR & BIKE PATH	Bridge Deck	1150
0010	B-09-0246	T-HALLIE	USH 53 SB	STH 124NB	Bridge Deck	1136
0010	B-09-0247	T-HALLIE	USH 53 NB	STH 124	Bridge Deck	1365
0010	B-09-0046	T-HALLIE	40TH AVE.	USH 53	Bridge Column	32
0010	B-09-0047	T-HALLIE	50TH AVE.	USH 53	Bridge Column	32
0010	B-09-0054	T-WHEATON	N	USH 53	Bridge Column	32
0010	B-09-0058	T-TILDEN	100TH AVE.	USH 53	Bridge Column	21
0010	B-09-0059	T-TILDEN	B	USH 53	Bridge Column	32
0010	B-09-0060	T-TILDEN	Q	USH 53	Bridge Column	32
0010	B-09-0063	T-WOODMOHR	Q	USH 53	Bridge Column	32
0010	B-09-0066	T-WOODMOHR	Q	USH 53	Bridge Column	32
0010	B-09-0067	T-WOODMOHR	190TH AVE.	USH 53	Bridge Column	21
0010	B-09-0068	T-BLOOMER	STH 64	USH 53	Bridge Column	32
0010	B-09-0078	C-BLOOMER	STH 40-17TH ST	USH 53	Bridge Column	32
0010	B-09-0091	V-NEW AUBURN	Q	USH 53	Bridge Column	32
0010	B-09-0092	V-NEW AUBURN	M MAIN ST	USH 53	Bridge Column	42
0010	B-09-0217	T-HALLIE	STH 29 WB	USH 53NB	Bridge Column	52
0010	B-09-0218	T-HALLIE	STH 29 EB	USH 53	Bridge Column	42
0010	B-09-0227	T-HALLIE	CTH OO	USH 53	Bridge Column	63
0010	B-09-0262	T-HALLIE	MELBY STREET	USH 53	Bridge Column	21
<b>CHIPPEWA COUNTY TOTAL</b>						8,827

PROTECTIVE SURFACE TREATMENT RESEAL						
						502.3215
						PROTECTIVE SURFACE TREATMENT RESEAL
CAT	STRUCTURE ID	VILLAGE, TOWN OR CITY	FEATURE ON	FEATURE UNDER	SURFACE TYPE	SY
<b>DOUGLAS COUNTY</b>						
0010	B-16-0012	C-SUPERIOR	USH 53-E 2ND ST	HILL AVE	Bridge Deck	4,700
0010	B-16-0030	T-AMNICON	USH 53 SB	MIDDLE RIVER	Bridge Deck	479
0010	B-16-0031	T-AMNICON	USH 53 NB	MIDDLE RIVER	Bridge Deck	459
0010	B-16-0033	T-AMNICON	USH 53 SB	AMNICON RIVER	Bridge Deck	614
0010	B-16-0034	T-AMNICON	USH 53 NB	AMNICON RIVER	Bridge Deck	611
0010	B-16-0048	T-AMNICON	USH 53 NB	USH 2 EB	Bridge Deck	779
0010	B-16-0079	T-GORDON	USH 53 SB	ST CROIX RIVER 14	Bridge Deck	816
0010	B-16-0080	T-SOLON SPRINGS	USH 53 NB	BEEBE CREEK	Bridge Deck	1,457
0010	B-16-0082	T-GORDON	USH 53 NB	ST CROIX RIVER 14	Bridge Column	922
0010	B-16-0095	T-SOLON SPRINGS	USH 53 S	WIS. CENTRAL RR	Bridge Deck	1,505
0010	B-16-00050005	C-SUPERIOR	IH 535-USH 53	USH 53 SB-STH 35	Bridge Deck	398
0010	B-16-0024	T-PARKLAND	STH 13	USH 2-USH 53	Bridge Deck	157
<b>DOUGLAS COUNTY TOTAL</b>						12,897

PROTECTIVE SURFACE TREATMENT RESEAL						
						502.3215
						PROTECTIVE SURFACE TREATMENT RESEAL
CAT	STRUCTURE ID	VILLAGE, TOWN OR CITY	FEATURE ON	FEATURE UNDER	SURFACE TYPE	SY
<b>EAU CLAIRE COUNTY</b>						
0010	B-18-0099	C-EAU CLAIRE	AA	USH 53	Bridge Column	53
0010	B-18-0159	C-ALTOONA	SPOONER AVE	USH 53	Bridge Column	21
0010	B-18-0165	C-ALTOONA	RIVER PRAIRIE DR	53	Bridge Column	40
0010	B-18-0173	C-ALTOONA	USH 12	USH 53	Bridge Column	73
0010	B-18-0176	C-EAU CLAIRE	RAMP STH 93	53	Bridge Column	12
0010	B-18-0186	T-SEYMOUR	QQ	USH 53	Bridge Column	42
0010	B-18-0187	T-SEYMOUR	TOWER DRIVE	53	Bridge Column	32
0010	B-18-0188	T-SEYMOUR	ORTH CROSSINGS(312	USH 53	Bridge Column	73
0010	B-18-0190	T-SEYMOUR	LASALLE ST	USH 53	Bridge Column	32
<b>EAU CLAIRE COUNTY TOTAL</b>						378

PROTECTIVE SURFACE TREATMENT RESEAL						
						502.3215
						PROTECTIVE SURFACE TREATMENT RESEAL
CAT	STRUCTURE ID	VILLAGE, TOWN OR CITY	FEATURE ON	FEATURE UNDER	SURFACE TYPE	SY
<b>WASHBURN COUNTY</b>						
0010	B-65-0026	T-TREGO	USH 53 SB	NAMEKAGON RIVER	Bridge Deck	510
0010	B-65-0028	T-MINONG	USH 53	TOTOGATIC RIVER	Bridge Deck	1,286
0010	B-65-0031	T-TREGO	USH 53 NB	NAMEKAGON RIVER	Bridge Deck	500
0010	B-65-0038	T-MINONG	USH 53	TOTOGATIC RIVER	Bridge Deck	1,617
0010	B-65-0030	T-SPOONER	STH 70	USH 53	Bridge Column	42
0010	B-65-0056	T-TREGO	USH 63	USH 53	BRIDGE DECK	622
0010	B-65-0056	T-TREGO	USH 63	USH 53	BRIDGE COLUMN	596
0010	B-65-0058	T-TREGO	USH 53	POTATO CREEK	BRIDGE DECK	1,962
0010	B-65-0059	T-TREGO	USH 53	POTATO CREEK	BRIDGE DECK	107
<b>WASHBURN COUNTY TOTAL</b>						7,242
<b>TOTAL 0010</b>						31,042

INLET PROTECTION TYPE B

628.7010  
INLET PROTECTION  
TYPE B

**NOTE:** INLET PROTECTION  
INLCUDES TWO INSTALLATIONS PER  
INLET. FIRST INSTALLATIONS ARE  
WITH TYPE FF FABRIC DURING DECK  
PREPARATION AND CLEANING  
ACTIVITIES. SECOND INSTALLATION IS  
WITH IMPERVIOUS FABRIC DURING  
SEALING OPEERATIONS. EACH PAID  
SEPARATELY UNDER ITEM 628.7010

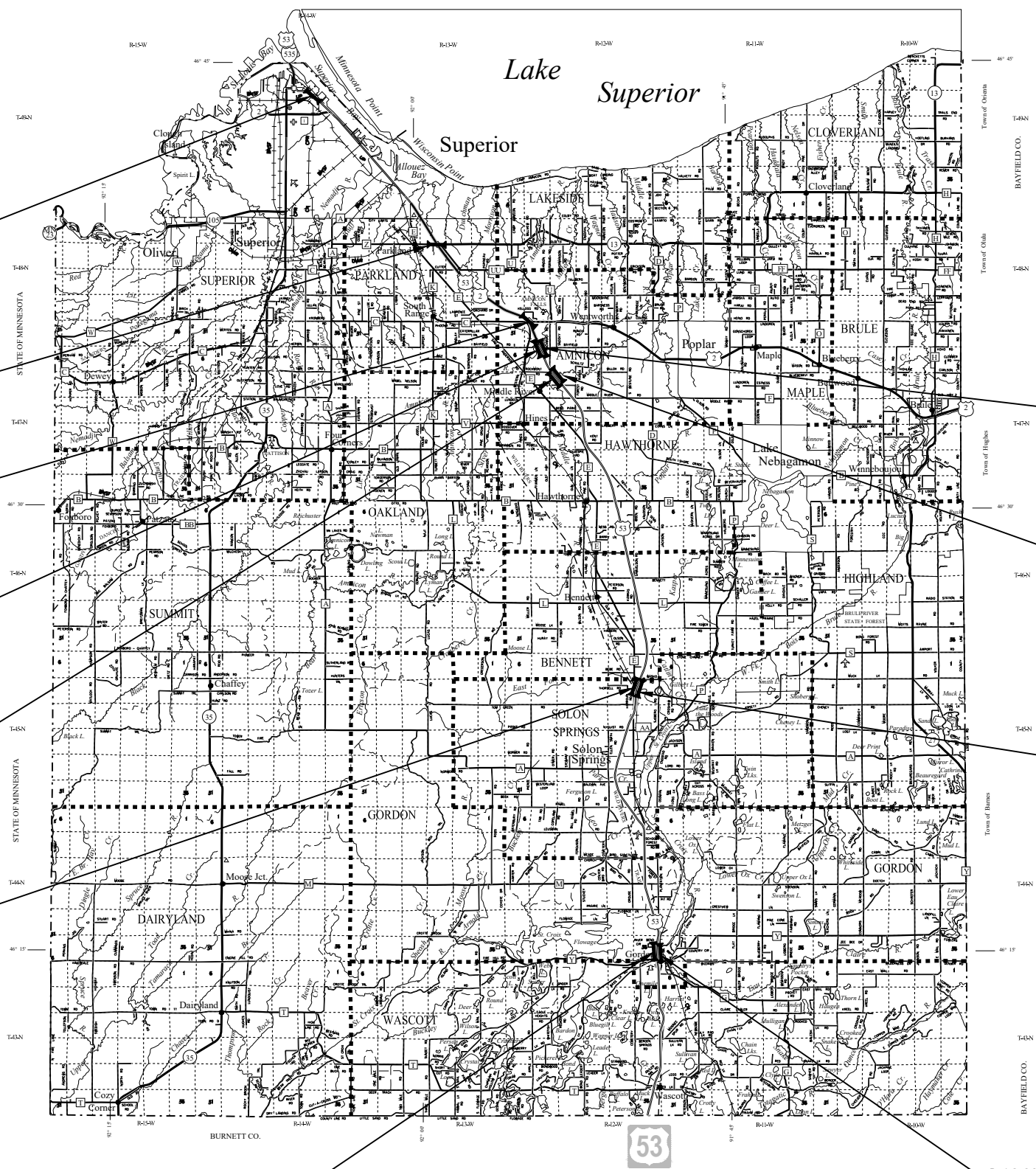
CAT	STRUCTURE ID	LOCATION	FEATURE ON	FEATURE UNDER	EACH
0010	B-09-0056	CTH S	USH 53 SB	CTH S	2
0010	B-09-0057	CTH S	USH 52 NB	CTH S	2
0010	B-09-0089	72ND STREET	USH 53 SB	72ND STREET	2
0010	B-09-0090	72ND STREET	STH 29 EB	72ND STREET	2
0010		UNDISTRIBUTED		UNDISTRIBUTED	2
TOTAL 0010					10

TRAFFIC CONTROL SUMMARY 1

CAT	COUNTY	STRUCTURE ID	DESCRIPTION	FEATURE ON	FEATURE UNDER	643.0300	643.0420	643.0705	643.0715	REMARKS
						TRAFFIC CONTROL DRUMS	TRAFFIC CONTROL BARRICADES TYPE III	TRAFFIC CONTROL WARNING LIGHTS TYPE A	TRAFFIC CONTROL WARNING LIGHTS TYPE C	
0010	BARRON	B-03-0196	NB OFF RAMP TO CTH V	USH 53	N/A	48	6	12	25	OFF
0010	CHIPPEWA	B-09-0247	NB ON RAMP FROM STH 29	STH 29 WB	STH 124	144	7	14	64	PEAK
TOTAL 0010						192	13	26	89	

TRAFFIC CONTROL SUMMARY 2

CAT	COUNTY	STRUCTURE ID	DESCRIPTION	FEATURE ON	FEATURE UNDER	643.0800	643.0900	643.1051	643.5000	REMARKS
						TRAFFIC CONTROL ARROW BOARDS	TRAFFIC CONTROL SIGNS	TRAFFIC CONTROL SIGNS PCMS WITH CELLULAR COMMUNICATIONS	TRAFFIC CONTROL	
0010	BARRON	B-03-0196	NB OFF RAMP TO CTH V	USH 53	N/A	2	10	8	1	780
0010	CHIPPEWA	B-09-0247	NB ON RAMP FROM STH 29	STH 29 WB	N/A	2	10	8		780
TOTAL 0010						4	20	16	1	



B-16-012 ON USH 53 OVER HILL AVENUE  
BRIDGE DECK SEALING  
FOLLOW SDD 15D43

B-16-024 ON STH 13 OVER USH 53  
BRIDGE COLUMN SEALING FOLLOW SDD 15D43

B-16-048 ON USH 53  
OVER USH 2 EB  
BRIDGE DECK SEALING  
FOLLOW SDD 15D43

B-16-033 ON USH 53 SB  
OVER AMNICON RIVER  
BRIDGE DECK SEALING  
FOLLOW SDD 15D43

B-16-030 ON USH 53 SB  
OVER MIDDLE RIVER  
BRIDGE DECK SEALING  
FOLLOW SDD 15D43

B-16-095 ON USH 53 OVER  
WISCONSIN CENTRAL RR  
AND BEEBE CREEK BRIDGE  
BRIDGE DECK SEALING  
FOLLOW SDD 15D43

B-16-079 ON USH 53 SB  
OVER ST CROIX RIVER  
BRIDGE DECK SEALING  
FOLLOW SDD 15D43

B-16-034 ON USH 53 NB  
OVER AMNICON RIVER  
BRIDGE DECK SEALING  
FOLLOW SDD 15D43

B-16-031 ON USH 53 NB  
OVER MIDDLE RIVER  
BRIDGE DECK SEALING  
FOLLOW SDD 15D43

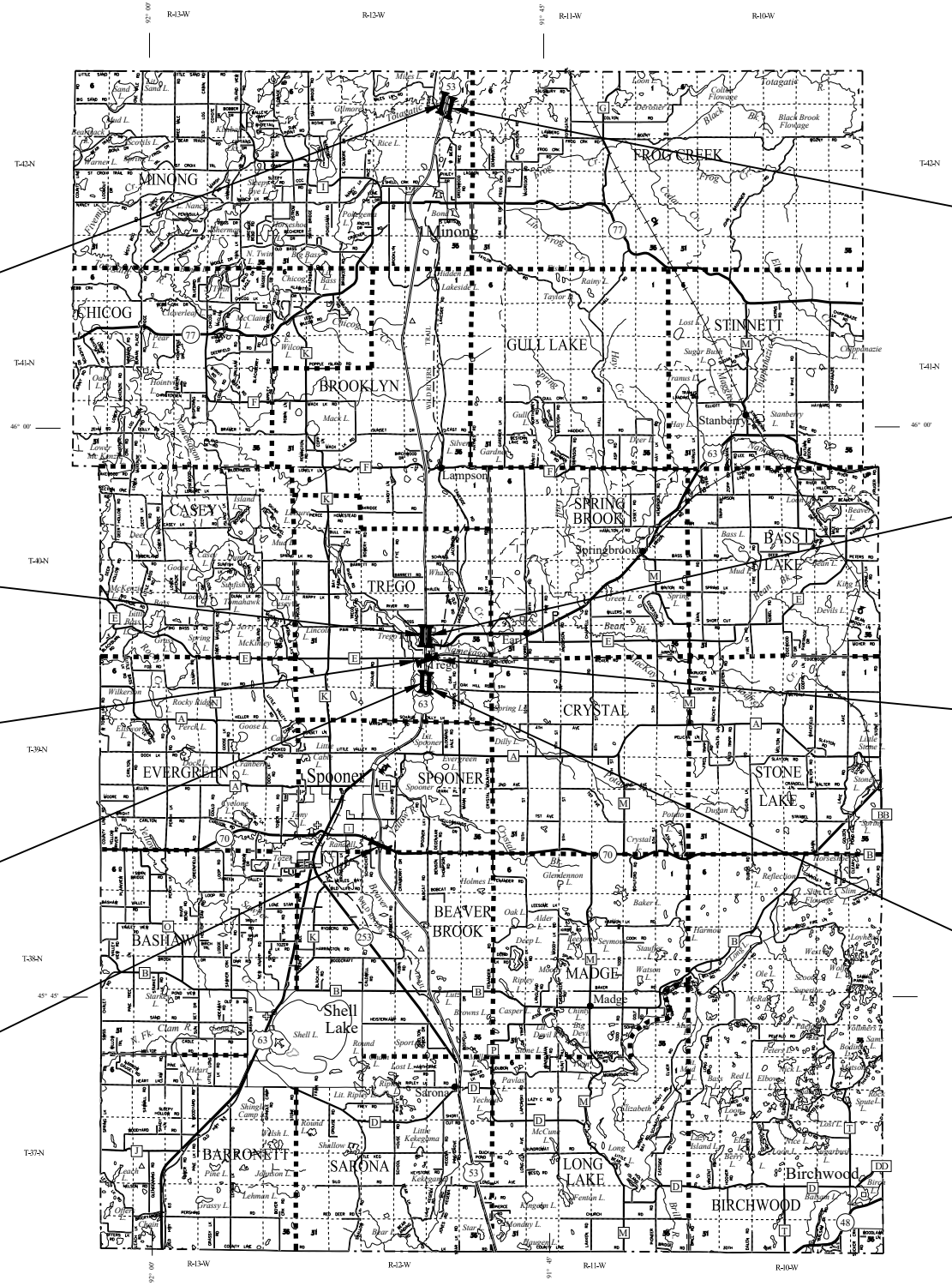
B-16-080 ON USH 53 NB OVER  
WISCONSIN CENTRAL LTD RR  
AND BEEBE CREEK  
BRIDGE DECK SEALING  
FOLLOW SDD 15D43

B-16-082 ON USH 53 NB  
OVER THE ST CROIX RIVER  
BRIDGE DECK SEALING  
FOLLOW SDD 15D43

5

5

PROJECT NO: 1190-01-85	HWY: USH 53	COUNTY: VARIOUS-SEE TITLE	DOUGLAS COUNTY CONCRETE SEALING PLAN	SHEET	E
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B-65-038 ON USH 53 SB  
OVER TATOGATIC RIVER  
BRIDGE DECK SEALING  
FOLLOW SDD 15D43

B-65-028 ON USH 53 NB  
OVER TOTAGATIC RIVER  
BRIDGE DECK SEALING  
FOLLOW SDD 15D43

B-65-026 ON USH 53 SB  
OVER NAMEKAGON RIVER  
BRIDGE DECK SEALING  
FOLLOW SDD 15D43

B-65-031 ON USH 53 NB  
OVER NAMEKAGON RIVER  
BRIDGE DECK SEALING  
FOLLOW SDD 15D43

B-65-056 ON USH 63  
OVER USH 53 NB & SB  
BRIDGE COLUMN SEALING  
FOLLOW SDD 15D43

B-65-056 ON USH 63  
OVER USH 53 NB & SB  
BRIDGE DECK SEALING  
FOLLOW SDD 15D43

B-65-058 ON USH 53 SB  
OVER POTATO CREEK  
BRIDGE DECK SEALING  
FOLLOW SDD 15D43

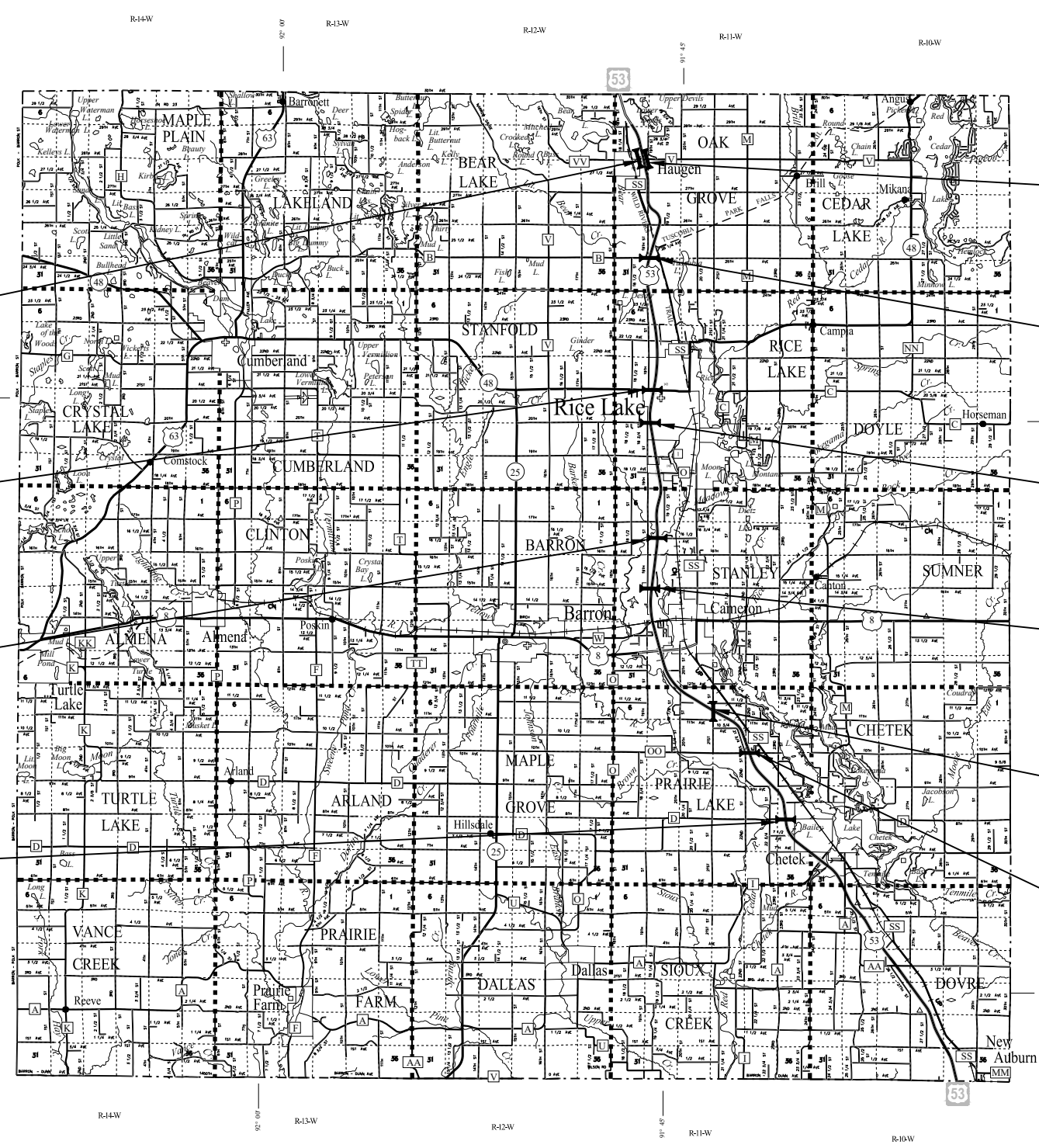
B-65-059 ON USH 53 NB  
OVER POTATO CREEK  
BRIDGE DECK SEALING  
FOLLOW SDD 15D43

B-65-030 ON STH 70  
OVER USH 53  
BRIDGE COLUMN SEALING  
FOLLOW SDD 15D43

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PROJECT NO: 1190-01-85	HWY: USH 53	COUNTY: VARIOUS-SEE TITLE	WASHBURN COUNTY CONCRETE SEALING PLAN	SHEET	E
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B-03-195 ON USH 53 SB  
OVER CTH V  
BRIDGE DECK SEALING  
FOLLOW SDD 15D43

B-03-047 ON 20 1/2 AVE. (AUGUSTA ST.)  
OVER USH 53  
BRIDGE COLUMN SEALING  
FOLLOW SDD 15D43

B-03-054 ON 16 1/2 AVE.  
OVER USH 53  
BRIDGE COLUMN SEALING  
FOLLOW SDD 15D43

B-03-027 ON CTH D  
OVER USH 53  
BRIDGE COLUMN SEALING  
FOLLOW SDD 15D43

B-03-196 ON USH 53 NB  
OVER CTH V  
BRIDGE DECK SEALING  
FOLLOW SDD 15D43

B-03-055 ON CTH B  
OVER USH 53  
BRIDGE COLUMN SEALING  
FOLLOW SDD 15D43

B-03-046 ON 20TH AVE. (ALLEN ST.)  
OVER USH 53  
BRIDGE COLUMN SEALING  
FOLLOW SDD 15D43

B-03-041 ON 15TH AVE.  
OVER USH 53  
BRIDGE COLUMN SEALING  
FOLLOW SDD 15D43

B-03-029 ON 21ST STREET  
OVER USH 53  
BRIDGE COLUMN SEALING  
FOLLOW SDD 15D43

B-03-028 ON CTH 00  
OVER USH 53  
BRIDGE COLUMN SEALING  
FOLLOW SDD 15D43

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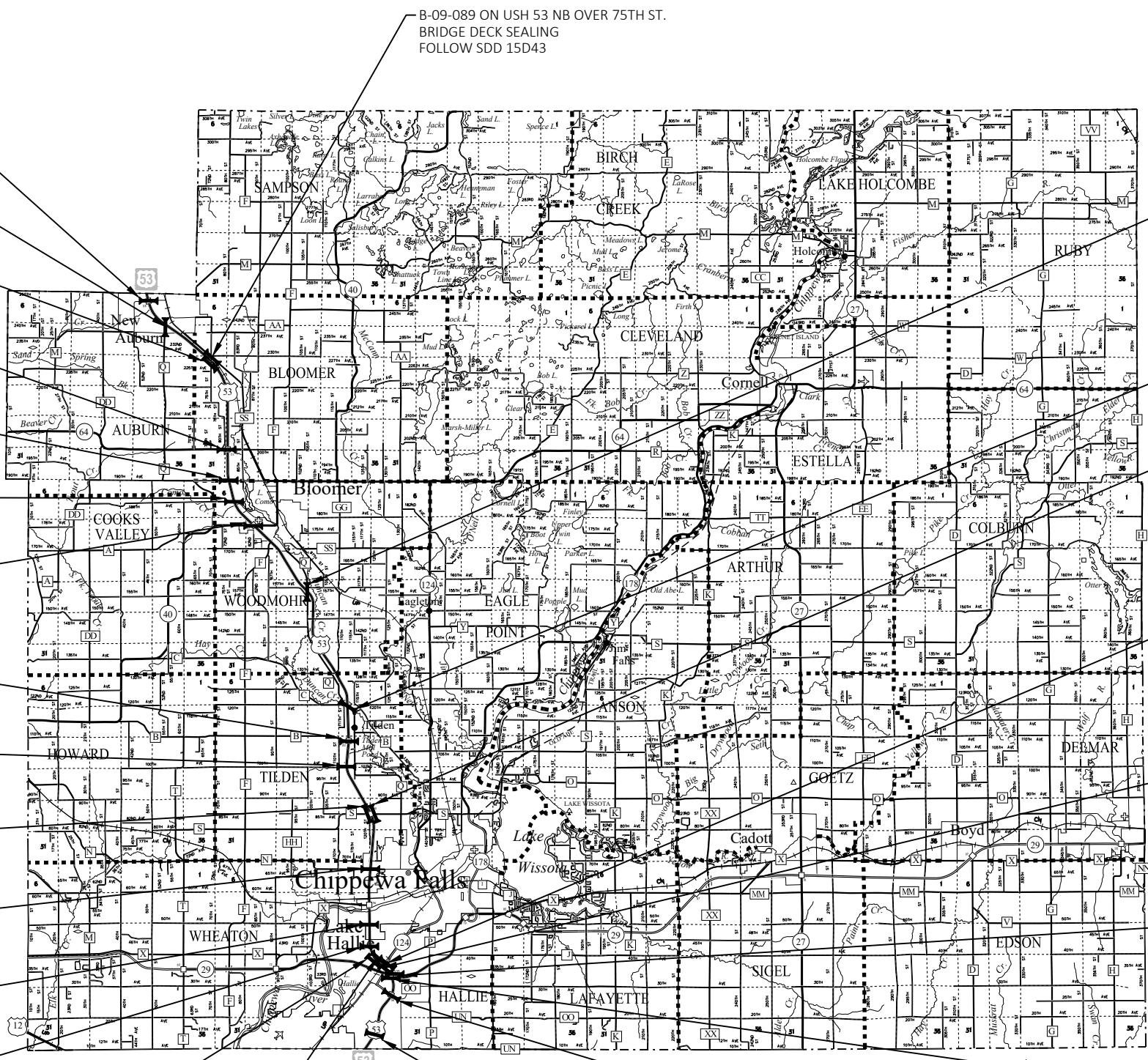
PROJECT NO: 1190-01-85	HWY: USH 53	COUNTY: VARIOUS-SEE TITLE	BARRON COUNTY CONCRETE SEALING PLAN	SHEET	E
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B-09-092 ON CTH M (MAIN ST.) OVER USH 53  
BRIDGE COLUMN SEALING  
FOLLOW SDD 15D43

B-09-091 ON CTH Q OVER USH 53  
BRIDGE COLUMN SEALING  
FOLLOW SDD 15D43

B-09-090 ON USH 53 SB OVER 75TH ST.  
BRIDGE DECK SEALING  
FOLLOW SDD 15D43

B-09-68 ON STH 64 OVER USH 53  
BRIDGE COLUMN SEALING  
FOLLOW SDD 15D43

B-09-067 ON 190TH AVE. OVER USH 53  
BRIDGE COLUMN SEALING  
FOLLOW SDD 15D43

B-09-066 ON CTH Q OVER USH 53  
BRIDGE COLUMN SEALING  
FOLLOW SDD 15D43

B-09-078 ON STH 40 (17TH ST.) OVER USH 53  
BRIDGE COLUMN SEALING  
FOLLOW SDD 15D43

B-09-059 ON CTH B OVER USH 53  
BRIDGE COLUMN SEALING  
FOLLOW SDD 15D43

B-09-058 ON 100TH AVE. OVER USH 53  
BRIDGE COLUMN SEALING  
FOLLOW SDD 15D43

B-09-056 ON USH 53 SB OVER CTHS  
BRIDGE DECK SEALING  
FOLLOW SDD 15D43

B-09-054 ON CTH N OVER USH 53  
BRIDGE COLUMN SEALING  
FOLLOW SDD 15D43

B-09-047 ON 50TH AVE. (NELSON ROAD) OVER USH 53  
BRIDGE COLUMN SEALING  
FOLLOW SDD 15D43

B-09-046 ON 40TH AVE. OVER USH 53  
BRIDGE COLUMN SEALING  
FOLLOW SDD 15D43

B-09-0228 ON USH 53 SB OVER UNION PACIFIC RR  
BRIDGE DECK SEALING  
FOLLOW SDD 15D43

B-09-229 ON USH 53 NB OVER UNION PACIFIC RR  
BRIDGE DECK SEALING  
FOLLOW SDD 15D43

B-09-089 ON USH 53 NB OVER 75TH ST.  
BRIDGE DECK SEALING  
FOLLOW SDD 15D43

B-09-063 ON CTH Q OVER USH 53  
BRIDGE COLUMN SEALING  
FOLLOW SDD 15D43

B-09-060 ON CTH Q OVER USH 53  
BRIDGE COLUMN SEALING  
FOLLOW SDD 15D43

B-09-057 ON USH 53 NB OVER CTH S  
BRIDGE DECK SEALING  
FOLLOW SDD 15D43

B-09-247 ON USH 53 NB OVER STH 124  
BRIDGE DECK SEALING  
FOLLOW SDD 15D43

B-09-246 ON USH 53 SB OVER STH 124  
BRIDGE DECK SEALING  
FOLLOW SDD 15D43

B-09-217 ON STH 29 WB OVER USH 53  
BRIDGE COLUMN SEALING  
FOLLOW SDD 15D43

B-09-218 ON STH 29 EB OVER USH 53  
BRIDGE COLUMN SEALING  
FOLLOW SDD 15D43

B-09-227 ON CTH OO OVER USH 53  
BRIDGE COLUMN SEALING  
FOLLOW SDD 15D43

B-09-262 ON MELBY ST. OVER USH 53  
BRIDGE COLUMN SEALING  
FOLLOW SDD 15D43

PROJECT NO: 1190-01-85	HWY: USH 53	COUNTY: VARIOUS-SEE TITLE	CHIPPEWA COUNTY CONCRETE SEALING PLAN	SHEET	E
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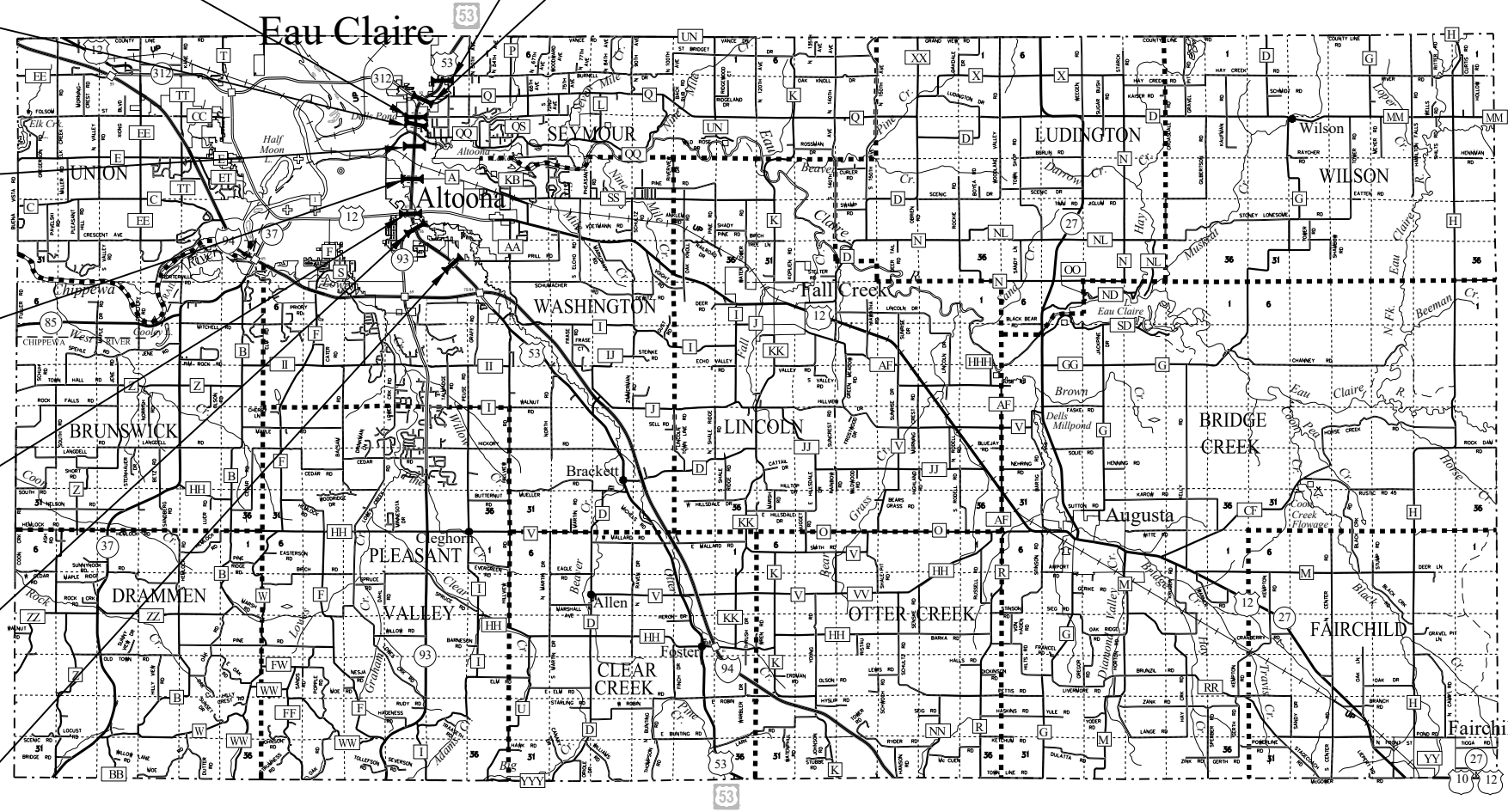
B-18-187 ON TOWER DRIVE  
OVER USH 53  
BRIDGE COLUMN SEALING  
FOLLOW SDD 15D43

B-18-190 ON LASALLE STREET  
OVER USH 53  
BRIDGE COLUMN SEALING  
FOLLOW SDD 15D43

B-18-186 ON CTH QQ  
OVER USH 53  
BRIDGE COLUMN SEALING  
FOLLOW SDD 15D43

B-18-188 ON NORTH CROSSING  
OVER USH 53  
BRIDGE COLUMN SEALING  
FOLLOW SDD 15D43

B-18-165 ON RIVER PRARIE DRIVE  
OVER USH 53  
BRIDGE COLUMN SEALING  
FOLLOW SDD 15D43



B-18-159 ON CTH A (SPOONER AVE.)  
OVER USH 53  
BRIDGE COLUMN SEALING  
FOLLOW SDD 15D43

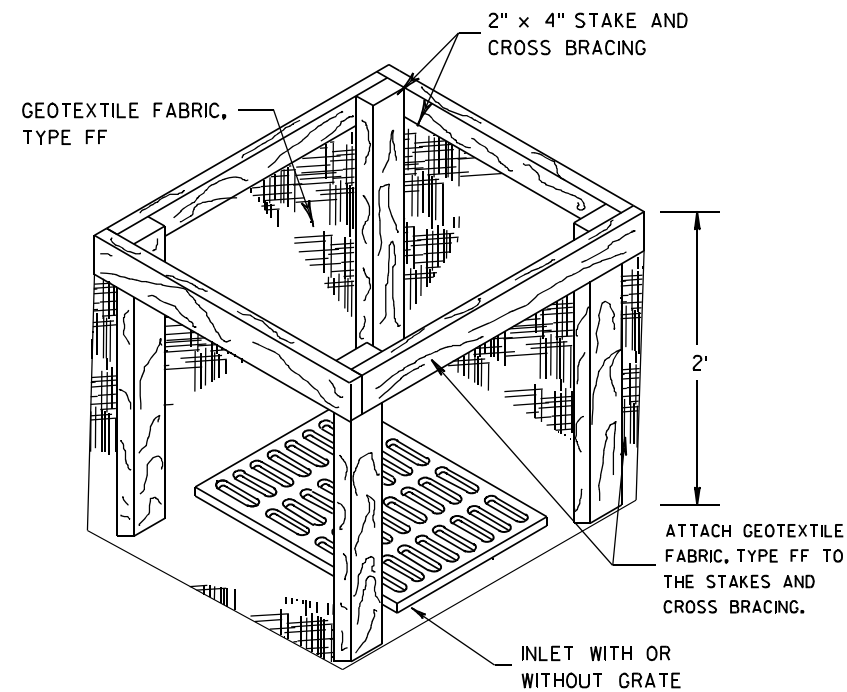
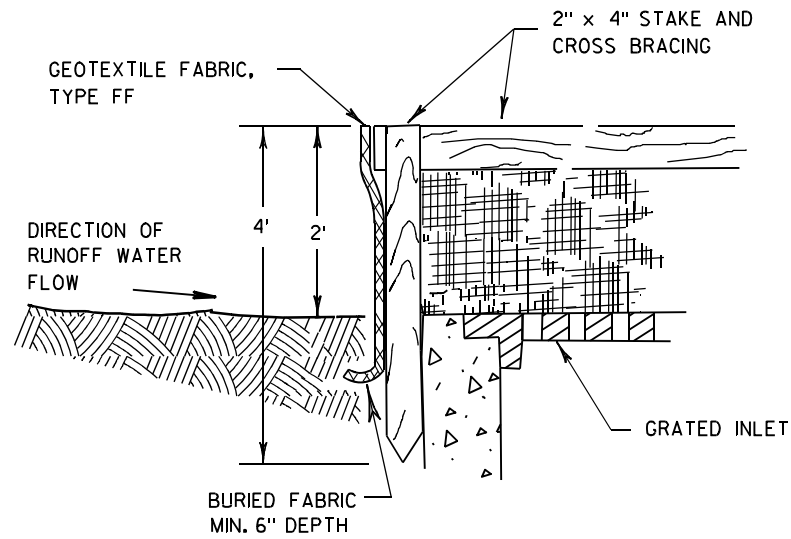
B-173 ON USH 12  
OVER USH 53  
BRIDGE COLUMN SEALING  
FOLLOW SDD 15D43

B-18-176 ON STH 93  
OVER USH 53  
BRIDGE COLUMN SEALING  
FOLLOW SDD 15D43

B-18-099 ON CTH AA (GOLF ROAD)  
OVER USH 53  
BRIDGE COLUMN SEALING  
FOLLOW SDD 15D43

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**INLET PROTECTION, TYPE A**

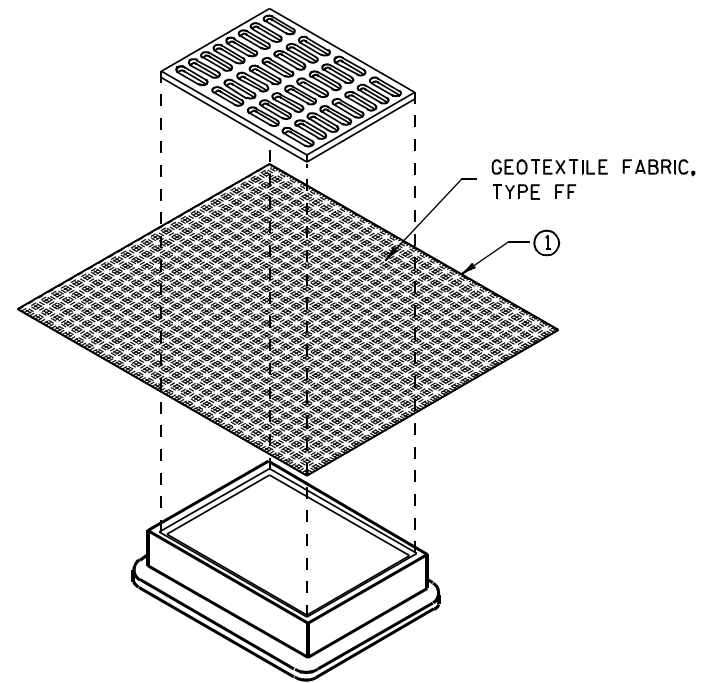
**GENERAL NOTES**

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

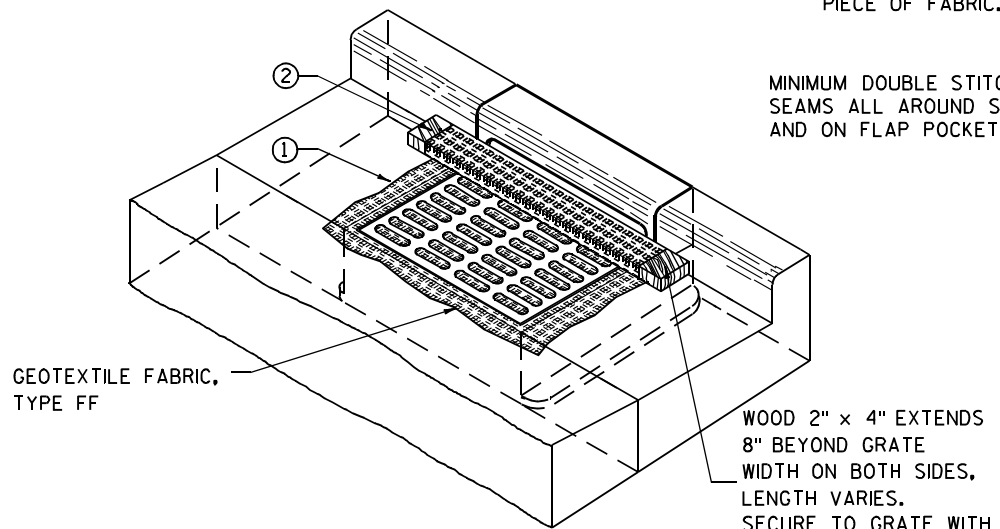
WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- ① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- ② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- ③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



**INLET PROTECTION, TYPE B  
(WITHOUT CURB BOX)**

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



**INLET PROTECTION, TYPE C (WITH CURB BOX)**

**INSTALLATION NOTES**

**TYPE B & C**

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

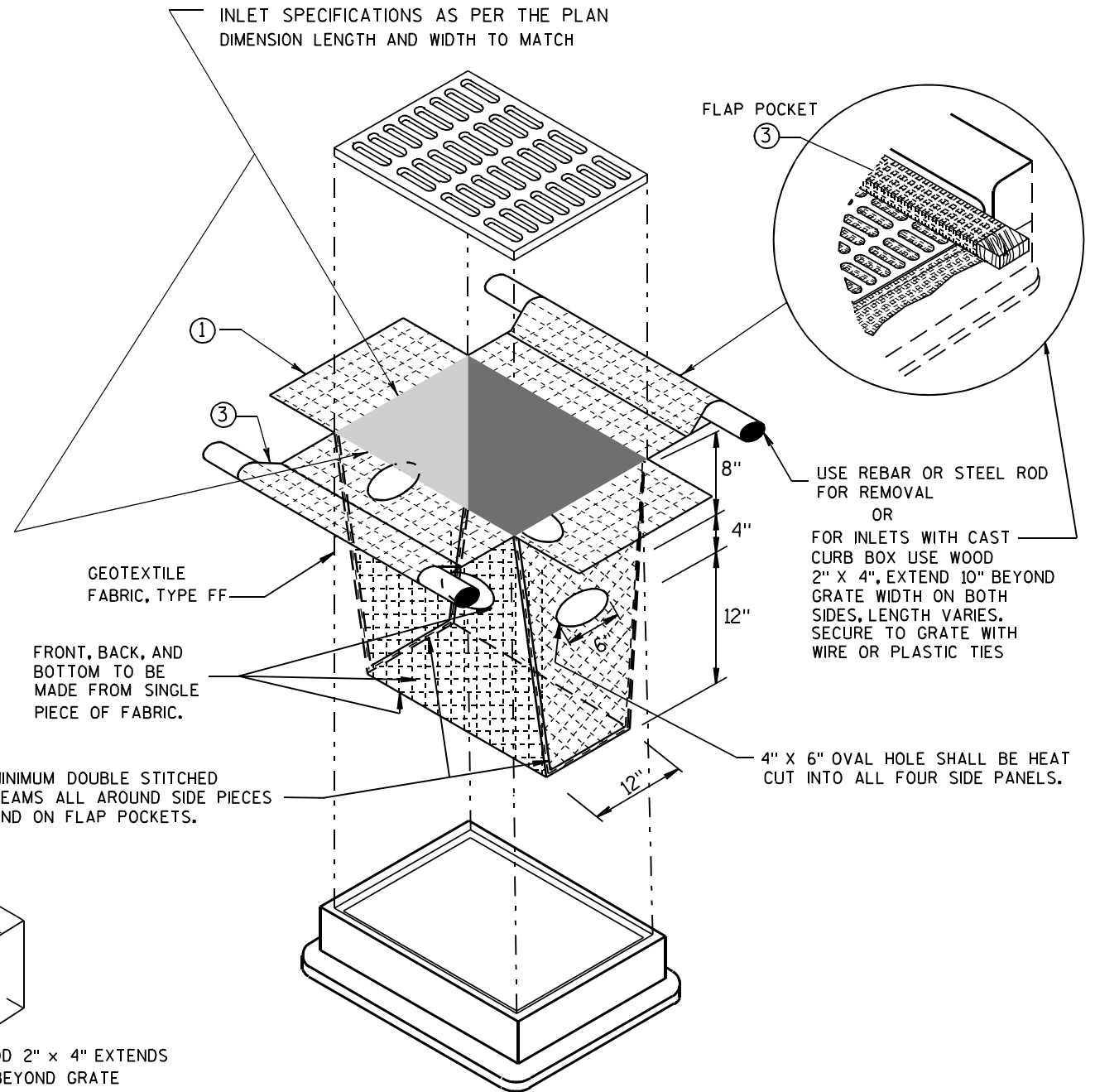
THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

**TYPE D**

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

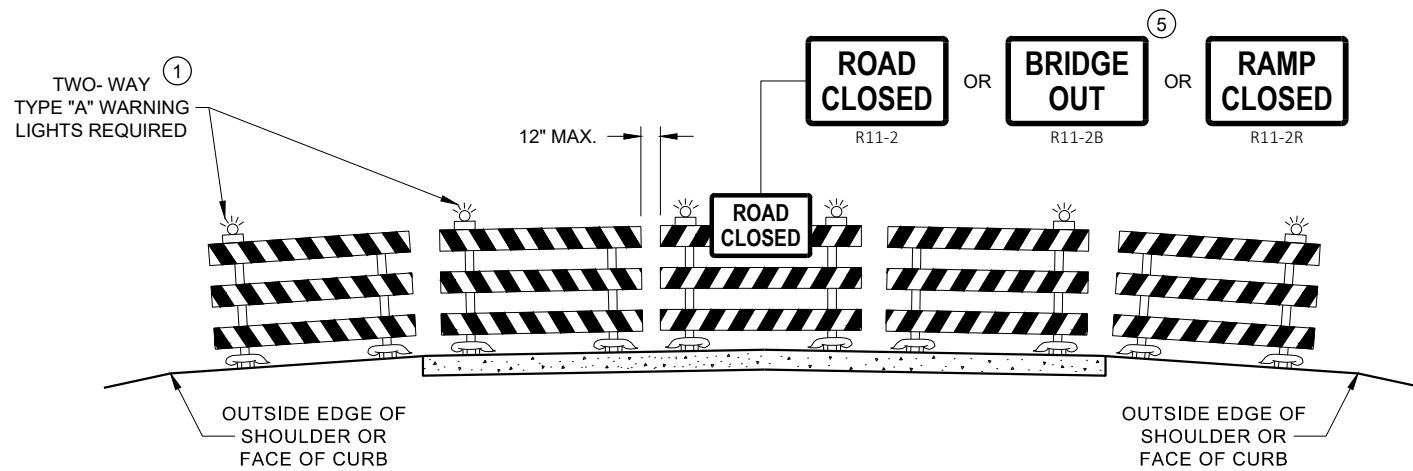
THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.



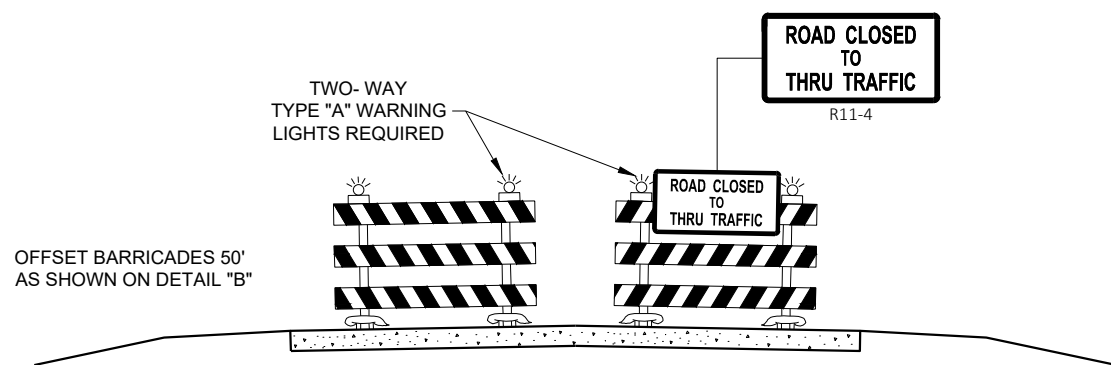
**INLET PROTECTION, TYPE D**

(CAN BE INSTALLED IN ANY INLET TYPE WITH OR WITHOUT A CURB BOX AS PER NOTE ②)

<b>INLET PROTECTION TYPE A, B, C, AND D</b>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 10/16/02 DATE	/s/ Beth Connestra CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA	



**DETAIL D  
ROAD CLOSURE BARRICADE DETAIL  
APPROACH VIEW**



**DETAIL E  
LANE CLOSURE BARRICADE DETAIL  
APPROACH VIEW**

SEE SDD 15C2 - SHEET "a" FOR LEGEND

**GENERAL NOTES**

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW - INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11 - 2, R11 - 3, M4 - 9, R11 - 4, AND R10 - 61 SIGNS PLACED ON THE BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE RAIL OR BOTTOM RAILS.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

- R11 - 2 SHALL BE 48" X 30"
- R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60" X 30"
- M4 - 9 SHALL BE 30" X 24"
- M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1 - 1 SHALL BE 36" X 36"

- ① TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT SPACING).
- ② THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.
- ③ FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".
- ④ FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".
- ⑤ FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.
- ⑥ INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.
- ⑦ "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

**BARRICADES AND SIGNS  
FOR  
VARIOUS CLOSURES**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
May 2023 /S/ Andrew Heidtke  
DATE WORK ZONE ENGINEER

THIS DRAWING PROVIDES GENERAL GUIDANCE ON TYPICAL TO SIGN LAYOUT AND SPACING. SEE PROJECT TO SIGNING SHEETS FOR SPECIFIC DETAILS FOR EACH PROJECT.

**LEGEND**

- ⊙ SIGN ON PERMANENT SUPPORT
- TO MO4 - 5
- EAST M3 - X
- XX M1 - 6 OR XX M1 - 4 OR XX M1 - 1
- ↶ M05 - 1 OR → M06 - 1 OR ↑ M06 - 1

**GENERAL NOTES**

- THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.
- IF THERE ARE ANY ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE TO ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. SEE SPECIFIC PROJECT TO SIGNING DETAIL SHEETS. MODIFY EXISTING SIGNS WHERE POSSIBLE.
- THE SPACING BETWEEN TRAFFIC CONTROL AND TO SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.
- ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.
- SIGNS THAT SHALL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.
- "MO" SIGNS ARE THE SAME AS "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.
- SIGN SIZES SHALL BE AS FOLLOW:  
 M3 - X SHALL BE 24" X 12". (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS).  
 MO4 - 5 SHALL BE 24" X 12". (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS).  
 M1 - 1, M1 - 4, AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS).  
 MO5 - 1, MO5 - 2, AND MO6 - 1, SHALL BE 21" X 21". (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS).  
 W20 - 53A SHALL BE 48" X 48"
- \* PLACE "RAMP CLOSED BEGINNING" SIGN 7 CALENDAR DAYS PRIOR TO CLOSURE OR AS DIRECTED BY THE ENGINEER. SEE WISCONSIN STANDARD SIGN PLATES FOR LAYOUT.

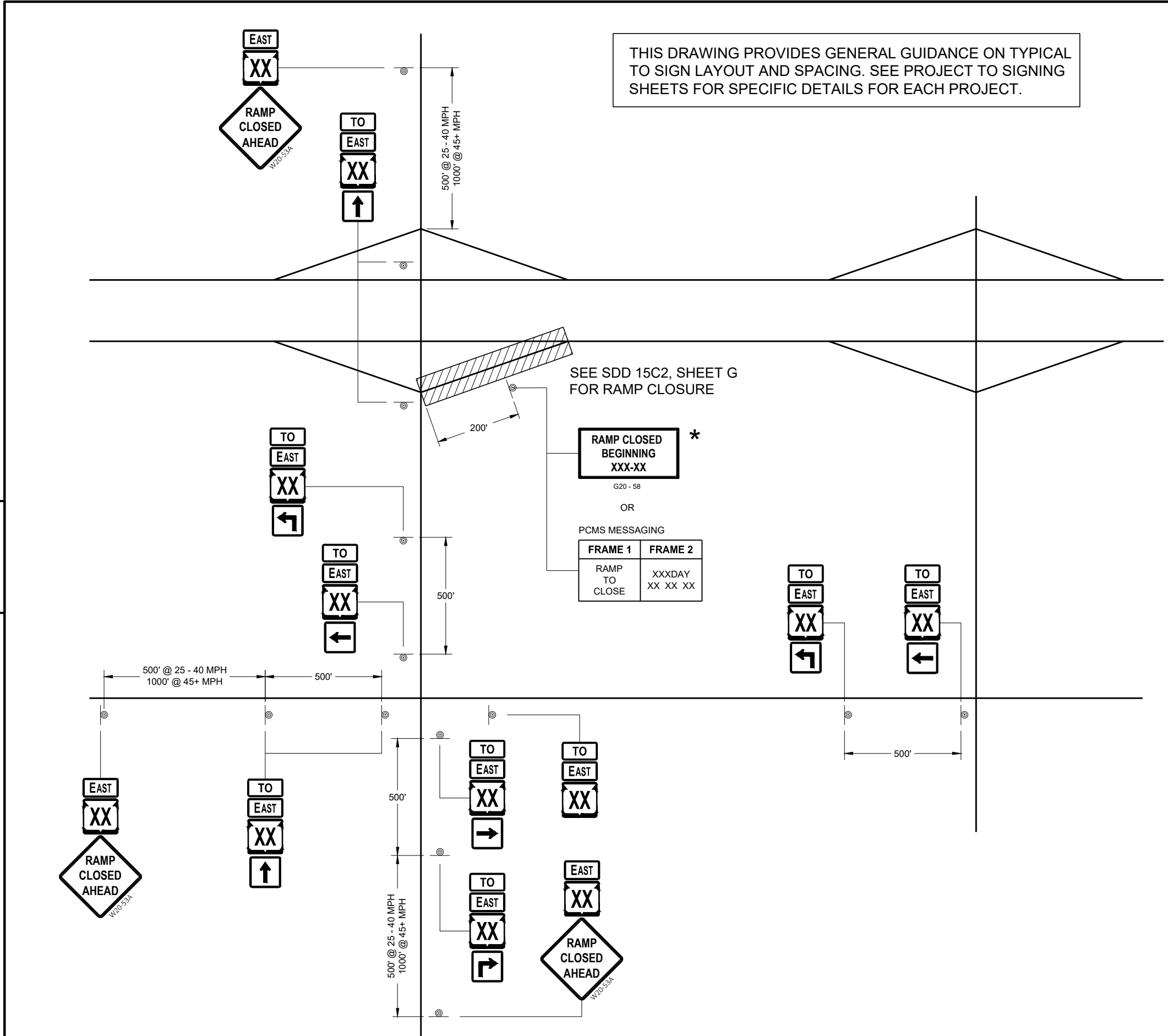
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SDD 15C02 - 09d

SDD 15C02 - 09d

<b>ON RAMP LANE CLOSURE</b>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	



THIS DRAWING PROVIDES GENERAL GUIDANCE ON TYPICAL "TO" MO-4 SIGN LAYOUT AND SPACING. SEE PROJECT TO SIGNING SHEETS FOR SPECIFIC DETAILS FOR EACH PROJECT.

**LEGEND**

- ⊙ SIGN ON PERMANENT SUPPORT
- PCMS PORTABLE CHANGEABLE MESSAGE SIGN
- TO MO4 - 5
- M1 - 4 OR M1 - 6 OR M1 - 5A
- M05 - 1 OR M05 - 2 OR M06 - 1 OR M06 - 2 OR M06 - 4

SEE SDD 15D16 FOR RAMP CLOSURE

**GENERAL NOTES**

- SEE SDD 15D16 "TRAFFIC CONTROL, EXIT RAMP CLOSURE" DETAIL FOR TRAFFIC CONTROL AT EXIT RAMP CLOSURE.
- THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.
- IF THERE ARE ANY ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE "TO" MO-4 ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. SEE SPECIFIC PROJECT TO SIGNING DETAIL SHEETS. MODIFY EXISTING SIGNS WHERE POSSIBLE.
- THE SPACING BETWEEN TRAFFIC CONTROL AND "TO" MO-4 SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.
- ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.
- SIGNS THAT SHALL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.
- "MO" SIGNS ARE THE SAME AS "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.
- SIGN SIZES SHALL BE AS FOLLOW:  
 MO4 - 5 SHALL BE 24" X 12". (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS).  
 M1 - 4, M1 - 5A, AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS).  
 MO5 - 1, MO5 - 2, AND MO6 - 1, SHALL BE 21" X 21". (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS).
- ① ONLY ADD IF THERE ARE NO EXISTING ROUTE MARKERS FOR THE INTERSECTING ROADWAY.

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SDD 15C02 - 09e

SDD 15C02 - 09e

PCMS MESSAGING

FRAME 1	FRAME 2
EXIT XX CLOSED	USE EXIT XX

OR

FIXED MESSAGE SIGN

HWY XX  
RAMP CLOSED  
USE EXIT XX

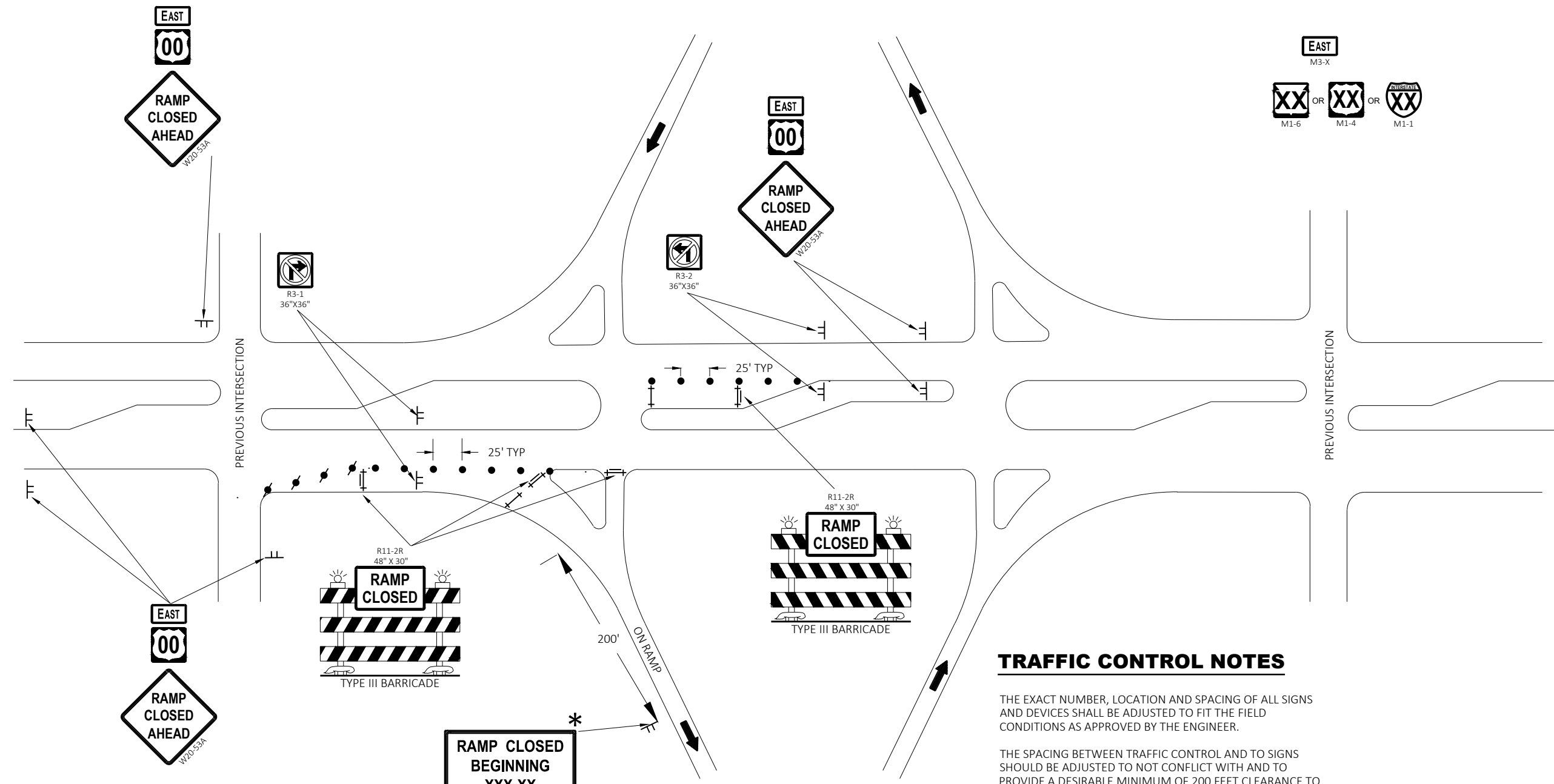
G20 - 56

**OFF RAMP  
LANE CLOSURE**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
May 2023 /S/ Andrew Heidtke  
DATE WORK ZONE ENGINEER

FHWA



**LEGEND**

- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- ↑ TYPE III BARRICADE
- ↑ TYPE III BARRICADE WITH ATTACHED SIGN
- ↑ SIGN ON PERMANENT SUPPORT
- ↑ SIGN ON TEMPORARY SUPPORT
- ↓ DIRECTION OF TRAFFIC

**RAMP CLOSED BEGINNING  
XXX-XX**  
G20-58

OR  
PCMS MESSAGING

FRAME 1	FRAME 2
RAMP TO CLOSE	XXX DAY XX XX XX

**TRAFFIC CONTROL FOR ENTRANCE RAMP CLOSURE**

**TRAFFIC CONTROL NOTES**

- THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT THE FIELD CONDITIONS AS APPROVED BY THE ENGINEER.
- THE SPACING BETWEEN TRAFFIC CONTROL AND TO SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.
- ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.
- SIGNS THAT SHALL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.
- SIGN SIZES SHALL BE AS FOLLOWS:
  - M3-X SHALL BE 24"x24" (36" x18" IF NEEDED TO MATCH EXISTING SIGNS)
  - M1-1, M1-4, AND M1-6 SHALL BE 24"x24" (36"x36" IF NEEDED TO MATCH EXISTING SIGNS)
  - W20-53A SHALL BE 48"x48"

\* PLACE "RAMP CLOSED BEGINNING" SIGN 7 CALENDAR DAYS PRIOR TO CLOSURE OR AS DIRECTED BY THE ENGINEER. SEE WISCONSIN STANDARD SIGN PLATES FOR LAYOUT.

<b>TRAFFIC CONTROL FOR ENTRANCE RAMP CLOSURE</b>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED May 2023 DATE	/S/ Andrew Heidtke WORK ZONE ENGINEER
FHWA	

**LEGEND**

- † TYPE III BARRICADE
- †† TYPE III BARRICADE WITH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- ⦿ SIGN ON PERMANENT SUPPORT
- ⚡ TYPE "A" WARNING LIGHT (FLASHING)
- ➡ DIRECTION OF TRAFFIC

**GENERAL NOTES**

THIS RAMP CLOSURE DETAIL IS TYPICAL FOR CLOSING A RIGHT SIDE EXIT RAMP. FOR A LEFT SIDE EXIT RAMP, REVERSE THE TRAFFIC CONTROL.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ANY SIGNS, TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

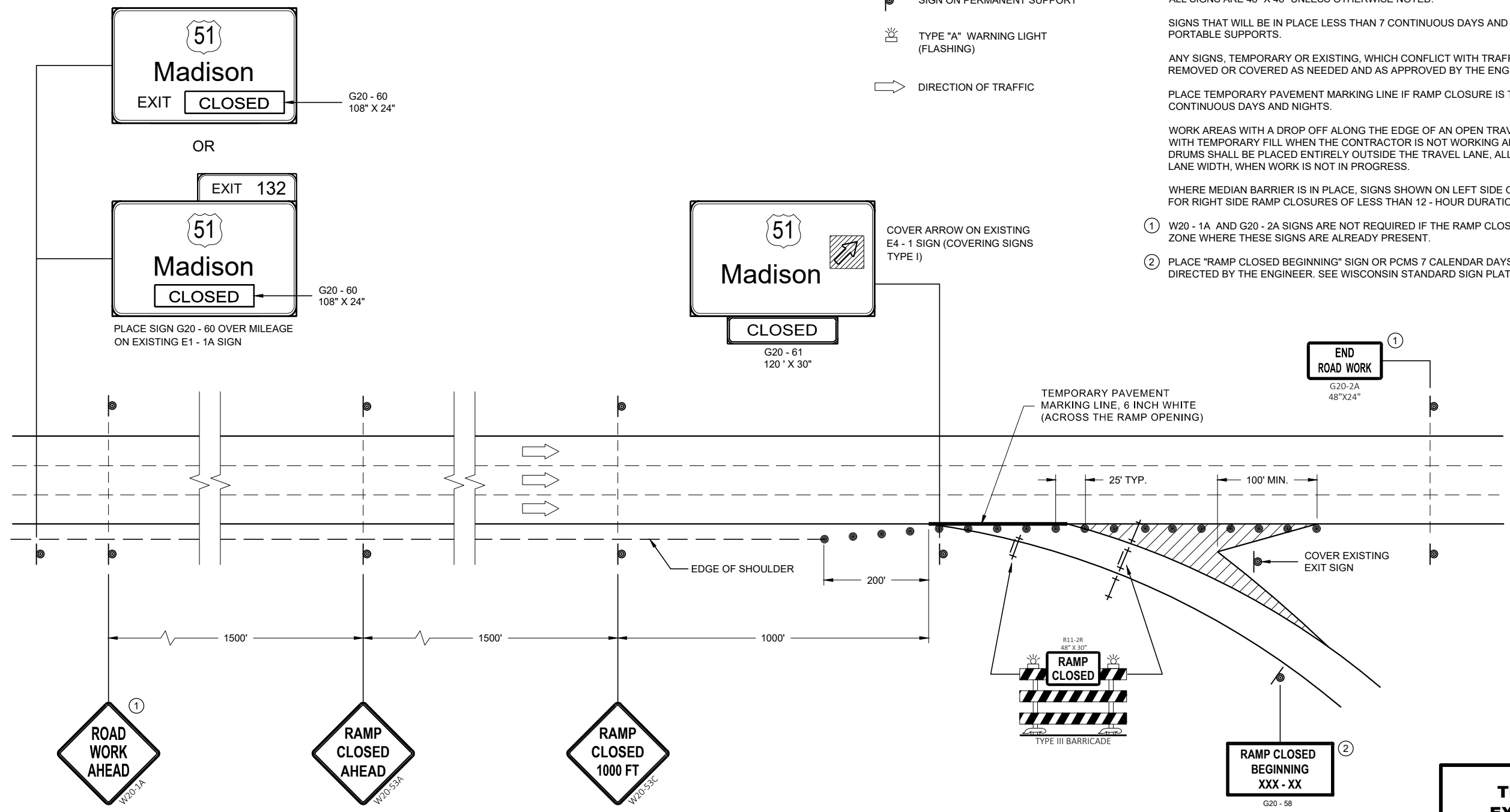
PLACE TEMPORARY PAVEMENT MARKING LINE IF RAMP CLOSURE IS TO BE IN PLACE 4 OR MORE CONTINUOUS DAYS AND NIGHTS.

WORK AREAS WITH A DROP OFF ALONG THE EDGE OF AN OPEN TRAVEL LANE SHALL BE LEVELED WITH TEMPORARY FILL WHEN THE CONTRACTOR IS NOT WORKING ADJACENT TO THE TRAVEL LANE. DRUMS SHALL BE PLACED ENTIRELY OUTSIDE THE TRAVEL LANE, ALLOWING THE FULL UNOBSTRUCTED LANE WIDTH, WHEN WORK IS NOT IN PROGRESS.

WHERE MEDIAN BARRIER IS IN PLACE, SIGNS SHOWN ON LEFT SIDE OF ROADWAY MAY BE OMITTED FOR RIGHT SIDE RAMP CLOSURES OF LESS THAN 12 - HOUR DURATION.

① W20 - 1A AND G20 - 2A SIGNS ARE NOT REQUIRED IF THE RAMP CLOSURE IS WITHIN A LARGER WORK ZONE WHERE THESE SIGNS ARE ALREADY PRESENT.

② PLACE "RAMP CLOSED BEGINNING" SIGN OR PCMS 7 CALENDAR DAYS PRIOR TO CLOSURE OR AS DIRECTED BY THE ENGINEER. SEE WISCONSIN STANDARD SIGN PLATES FOR SIGN LAYOUT.



**TRAFFIC CONTROL,  
EXIT RAMP CLOSURE**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
DATE May 2023 /S/ Andrew Heidtke  
ROADWAY STANDARDS DEVELOPMENT  
ENGINEER

FHWA

FRAME 1	FRAME 2
RAMP TO CLOSE	XXXDAY XX XX XX



### GENERAL NOTES

THIS DETAIL IS TYPICAL FOR CLOSING THE RIGHT SHOULDER. FOR CLOSING THE LEFT SHOULDER, REVERSE THE TRAFFIC CONTROL.

THIS DETAIL MAY BE USED FOR DIVIDED ROADWAYS WITH ANY NUMBER OF TRAVEL LANES.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE FHWA'S MANUAL OF STANDARD HIGHWAY SIGNS OR THE WISCONSIN STANDARD SIGN PLATES.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

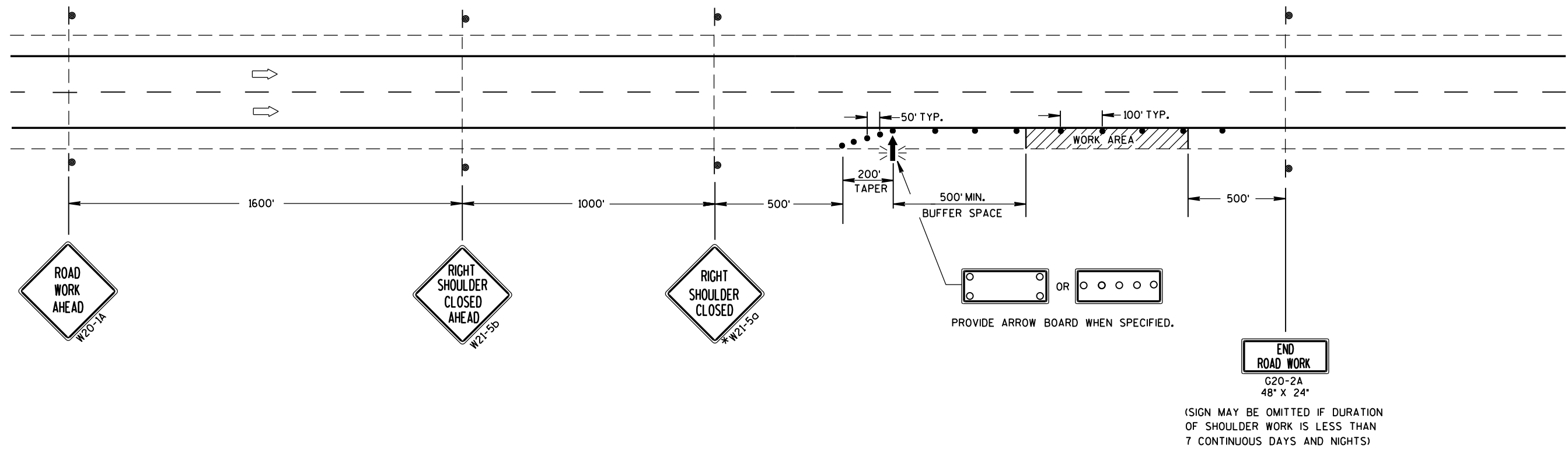
CHANNELIZING DEVICES PLACED ADJACENT TO THE WORK AREA SHALL BE PULLED BACK FROM THE TRAVEL LANE WHEN WORK IS NOT IN PROGRESS.

WHEN A RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

\*FOR SHORT DURATION SHOULDER WORK OF LESS THAN ONE HOUR, THE W21-50 SIGN MAY BE OMITTED.

### LEGEND

- TRAFFIC CONTROL DRUM
- ⊙ SIGN ON PERMANENT SUPPORT
- ➡ DIRECTION OF TRAFFIC
- ⚡ FLASHING ARROW BOARD
- ▨ WORK AREA

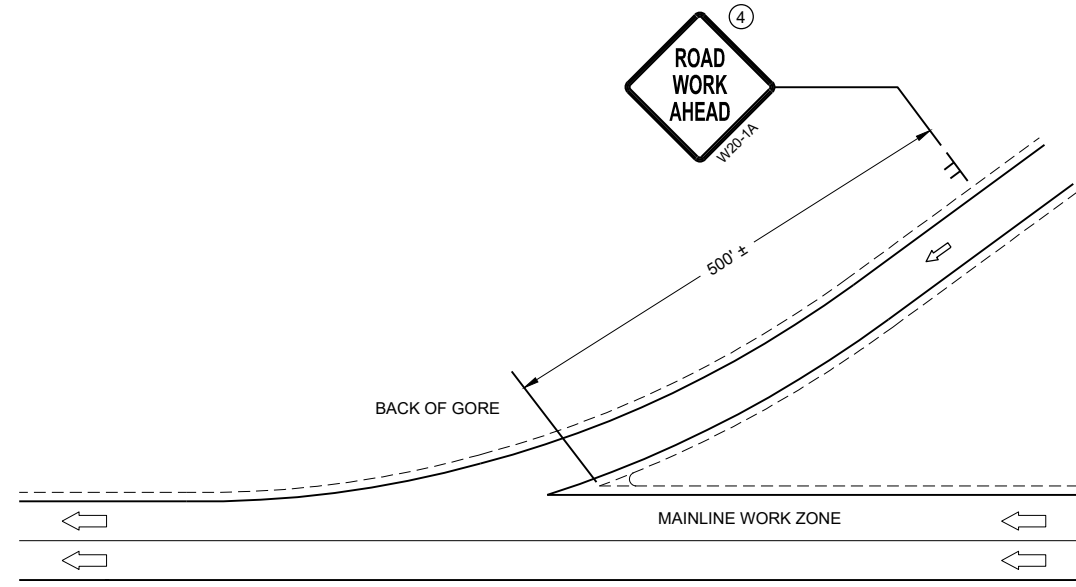


(SIGN MAY BE OMITTED IF DURATION OF SHOULDER WORK IS LESS THAN 7 CONTINUOUS DAYS AND NIGHTS)

<b>TRAFFIC CONTROL SHOULDER CLOSURE ON DIVIDED ROADWAY, SPEEDS GREATER THAN 40 MPH</b>	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED June 2016 DATE	/s/ Peter Amakobe Atepe STATEWIDE WORK ZONE TRAFFIC SAFETY ENGINEER
FHWA	

**LEGEND**

- V1 SHADOW VEHICLE 1
- V2 SHADOW VEHICLE 2
- V3 ADVANCE WARNING TRUCK
- TRAFFIC CONTROL DRUM
- ◻ TRUCK MOUNTED ATTENUATOR (TMA)
- ⊥ SIGN ON TEMPORARY SUPPORT
- ➡ DIRECTION OF TRAFFIC
- ◻ FLASHING ARROW PANEL (MERGE)
- ◻ FLASHING ARROW PANEL (CAUTION)
- PCMS PORTABLE CHANGEABLE MESSAGE SIGN
- ▨ WORK AREA



**GENERAL NOTES**

SHORT DURATION IS WORK THAT OCCUPIES A LOCATION UP TO 1 HOUR.

MOBILE IS WORK THAT MOVES INTERMITTENTLY OR CONTINUOUSLY.

ALL VEHICLES SHALL BE EQUIPPED WITH TWO 360 DEGREE HIGH INTENSITY YELLOW FLASHING LIGHTS OR STROBE LIGHTS AND OPERATED WITH HEADLIGHTS TURNED ON.

ALL VEHICLES SHALL BE EQUIPPED WITH REAR FACING TYPE B OR C FLASHING ARROW PANEL. SIGNS PLACED ON VEHICLES MUST NOT OBSCURE THE ARROW PANEL.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE SPECIFIED.

WHEN WORK ACTIVITY BLOCKS THE RIGHT LANE, REVERSE TRAFFIC CONTROL.

WHEN A RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, PROVIDE ADDITIONAL TRAFFIC CONTROLS AS SPECIFIED IN THE CONTRACT OR AS APPROVED BY THE ENGINEER.

USE DOUBLE ARROWS WHEN CONVOY IS IN CENTER LANE ONLY.

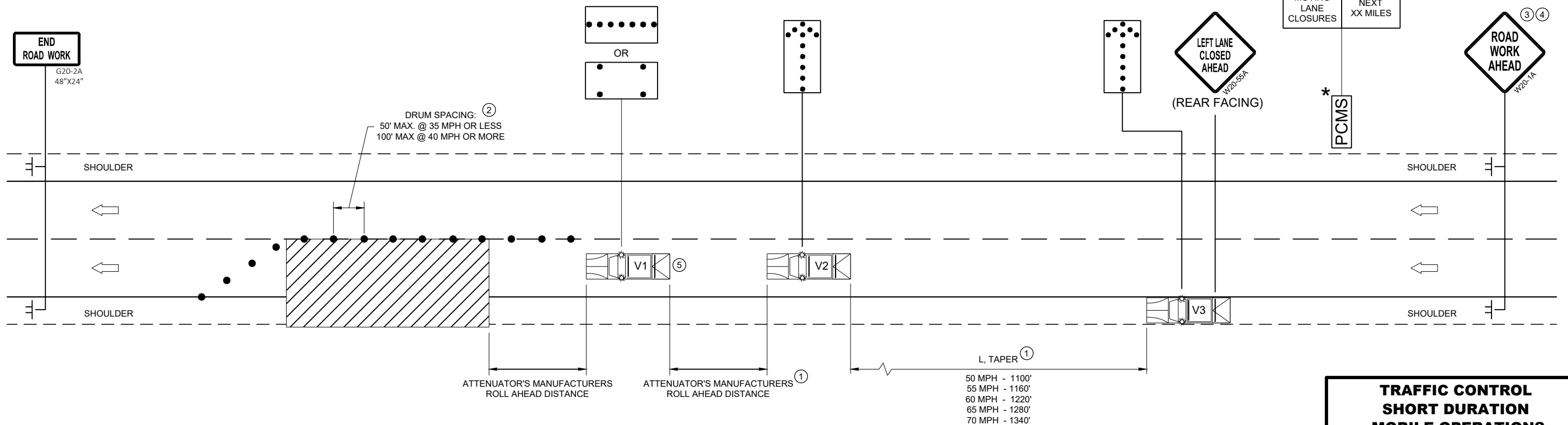
WHEN NO WORK ACTIVITY IS TAKING PLACE, REMOVE OR LAY STATIONARY SIGNS AND SUPPORTS FLAT ON THE GRADE WITH UPRIGHTS ORIENTED PARALLEL TO AND DOWNSTREAM FROM TRAFFIC

- ① DISTANCE BETWEEN VEHICLES MAY VARY ACCORDING TO TERRAIN, SIGHT DISTANCE AND OTHER FACTORS. WHENEVER ADEQUATE STOPPING SIGHT DISTANCE EXISTS TO THE REAR, SHADOW VEHICLES SHOULD MAINTAIN THE MINIMUM DISTANCE FROM THE WORK VEHICLE AND PROCEED AT THE SAME SPEED AS THE WORK VEHICLE. SHADOW VEHICLES SHOULD SLOW DOWN IN ADVANCE OF VERTICAL AND HORIZONTAL CURVES THAT RESTRICT SIGHT DISTANCE.
- ② DRUMS ARE TO BE USED FOR BRIDGE DECK SEALING AND OTHER PROJECTS THAT REQUIRE DELINEATION.
- ③ WITHIN 5 MILES, RELOCATE SIGNS AS WORK PROGRESSES AND NECESSARY OR AS DIRECTED BY THE ENGINEER.
- ④ SIGN NOT REQUIRED IF MOVING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.
- ⑤ SHADOW VEHICLE 1 (V1) IS OPTIONAL

\* PCMS OPTIONAL

PCMS MESSAGING

FRAME 1	FRAME 2
MOVING LANE CLOSURES	NEXT XX MILES



**TRAFFIC CONTROL  
SHORT DURATION  
MOBILE OPERATIONS**

STATE OF WISCONSIN  
DEPARTMENT OF TRANSPORTATION

APPROVED  
August 2021 /S/ Andrew Heidtke  
DATE WORK ZONE ENGINEER

FHWA

6

6

SDD 15D43 - 02

SDD 15D43 - 02

Notes



## ***Wisconsin Department of Transportation***

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