

SUP MAY 2024

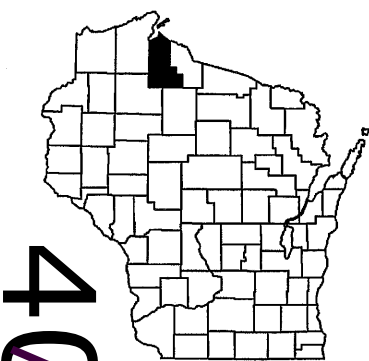
PROJECT ID: 8995-00-22

COUNTY: ASHLAND

ORDER OF SHEETS

Section No.	1	Title
Section No.	2	Typical Sections and Details
Section No.	3	Estimate of Quantities
Section No.	3	Miscellaneous Quantities
Section No.	4	Right of Way Plat
Section No.	5	Plan and Profile
Section No.	6	Standard Detail Drawings
Section No.	7	Sign Plates
Section No.	8	Structure Plans
Section No.	9	Computer Earthwork Data
Section No.	9	Cross Sections

TOTAL SHEETS = 24



40



DESIGN DESIGNATION

A.A.D.T.	2024	=	455
A.A.D.T.	2044	=	505
D.H.V.		=	60
D.D.		=	60/40
T.		=	N/A
DESIGN SPEED		=	30 MPH
ESALS		=	N/A

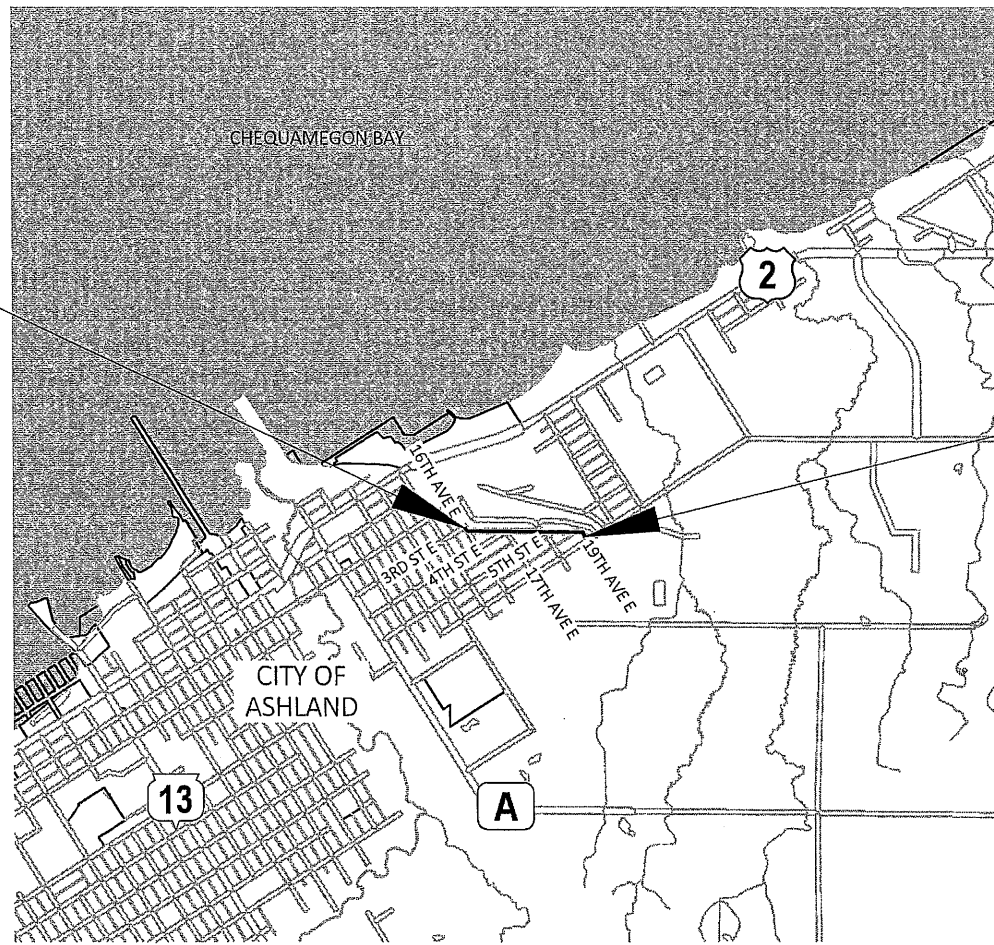
CONVENTIONAL SYMBOLS

PLAN	
CORPORATE LIMITS	
PROPERTY LINE	
LOT LINE	
LIMITED HIGHWAY EASEMENT	
EXISTING RIGHT OF WAY	
PROPOSED OR NEW R/W LINE	
SLOPE INTERCEPT	
REFERENCE LINE	
EXISTING CULVERT	
PROPOSED CULVERT (Box or Pipe)	
COMBUSTIBLE FLUIDS	
MARSH AREA	
WOODED OR SHRUB AREA	

PROFILE	
GRADE LINE	
ORIGINAL GROUND	
MARSH OR ROCK PROFILE (To be noted as such)	
SPECIAL DITCH	
GRADE ELEVATION	
CULVERT (Profile View)	
UTILITIES	
ELECTRIC	
FIBER OPTIC	
GAS	
SANITARY SEWER	
STORM SEWER	
TELEPHONE	
WATER	
UTILITY PEDESTAL	
POWER POLE	
TELEPHONE POLE	

BEGIN PROJECT
STA 10+51.16
Y = 325679.5560
X = 506285.4398

END PROJECT
STA 28+63.51
Y = 325512.9855
X = 508032.2148



SCALE 0 0.5 Mi
TOTAL NET LENGTH OF CENTERLINE = 0.343 MILES

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COUNTY COORDINATE SYSTEM (WCCS), ASHLAND COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES.

ELEVATIONS ARE REFERENCED TO NAVD 88 (2007). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 18

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
PLAN OF PROPOSED IMPROVEMENT
C ASHLAND, CARY STREET
16TH AVE E TO 19TH AVE E
LOC STR
ASHLAND COUNTY

STATE PROJECT NUMBER
8995-00-22

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
8995-00-22	WISC 2024344	1

ACCEPTED FOR
CITY OF ASHLAND
Date: 1/29/24
Signature and Title of Official: [Signature] Director DPW

ORIGINAL PLANS PREPARED BY
Cedar corporation
MENOMONIE - MADISON - GREEN BAY - CEDARBURG
www.cedarcorp.com
WISCONSIN PROFESSIONAL ENGINEER
WILLIAM A. BETZIG
E-27689
HARTLAND, WI
DATE: 1-29-24 [Signature] (Professional Engineer Signature)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
PREPARED BY
Surveyor: CEDAR CORPORATION
Designer: CEDAR CORPORATION
Project Manager: PAULA GROOM, P.E.
Regional Examiner: TOU YANG, P.E.
Regional Supervisor: JEFFREY OLSON, P.E.

APPROVED FOR THE DEPARTMENT
DATE: 02/01/2024
Signature: [Signature] (Signature)

E

GENERAL NOTES

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

NO TREES OR SHRUBS ARE TO BE REMOVED UNLESS SUCH TREES OR SHRUBS HAVE FIRST BEEN INDICATED FOR REMOVAL BY THE ENGINEER IN THE FIELD.

WHEN THE QUANTITY OF ITEM BASE LAYER OR SURFACE LAYER IS MEASURED FOR PAYMENT BY THE TON, THE THICKNESS OF THE MATERIAL THAT IS SHOWN ON THE PLANS IS APPROXIMATE AND THE ACTUAL THICKNESS WILL DEPEND ON THE DISTRIBUTION OF MATERIAL AS DIRECTED BY THE ENGINEER.

STANDARD ABBREVIATIONS

Table with 4 columns: Abbreviation, Description, Abbreviation, Description. Includes terms like ABUT, AGG, ET AL, AADT, BF, BM, C/L OR CL, etc.

UTILITIES CONTACTS

WISCONSIN DNR LIAISON

SHAWN HASELEU
WEST NORTHERN REGION
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SPOONER, WI 54801
PHONE: (715) 416-0478
EMAIL: SHAWN.HASELEU@WISCONSIN.GOV

DESIGN CONSULTANT

CEDAR CORPORATION
DENNIS MACK, P.E.
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MENOMONIE, WI 54751
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EMAIL: DENNIS.MACK@CEDARCORP.COM

MUNICIPALITY

CITY OF ASHLAND - WATER
JOHN BUTLER, P.E. - DIRECTOR
601 MAIN STREET W
ASHLAND, WI 54806
PHONE: (715) 685-1648
EMAIL: JBUTLER@COAWI.ORG

CITY OF ASHLAND - SEWER
JOHN BUTLER, P.E. - DIRECTOR
601 MAIN STREET W
ASHLAND, WI 54806
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GAS/ELECTRIC

XCEL ENERGY
BEN KOZAK
2400 FARM ROAD
ASHLAND, WI 54806
PHONE: (715) 682-6915
EMAIL: BENJAMIN.KOZAK@XCELENERGY.COM

COMMUNICATION

SPECTRUM COMMUNICATIONS
RYAN NELSON, CONSTRUCTION COORDINATOR, II
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ASHLAND, WI 54806
PHONE: (715) 979-5015
EMAIL: RYAN.NELSON@CHARTER.COM

COMMUNICATION

BRIGHTSPEED/LUMEN
MARK SCRIBNER
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LA CROSSE, WI 54601
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EMAIL: MARK.SCRIBNER@BRIGHTSPEED.COM

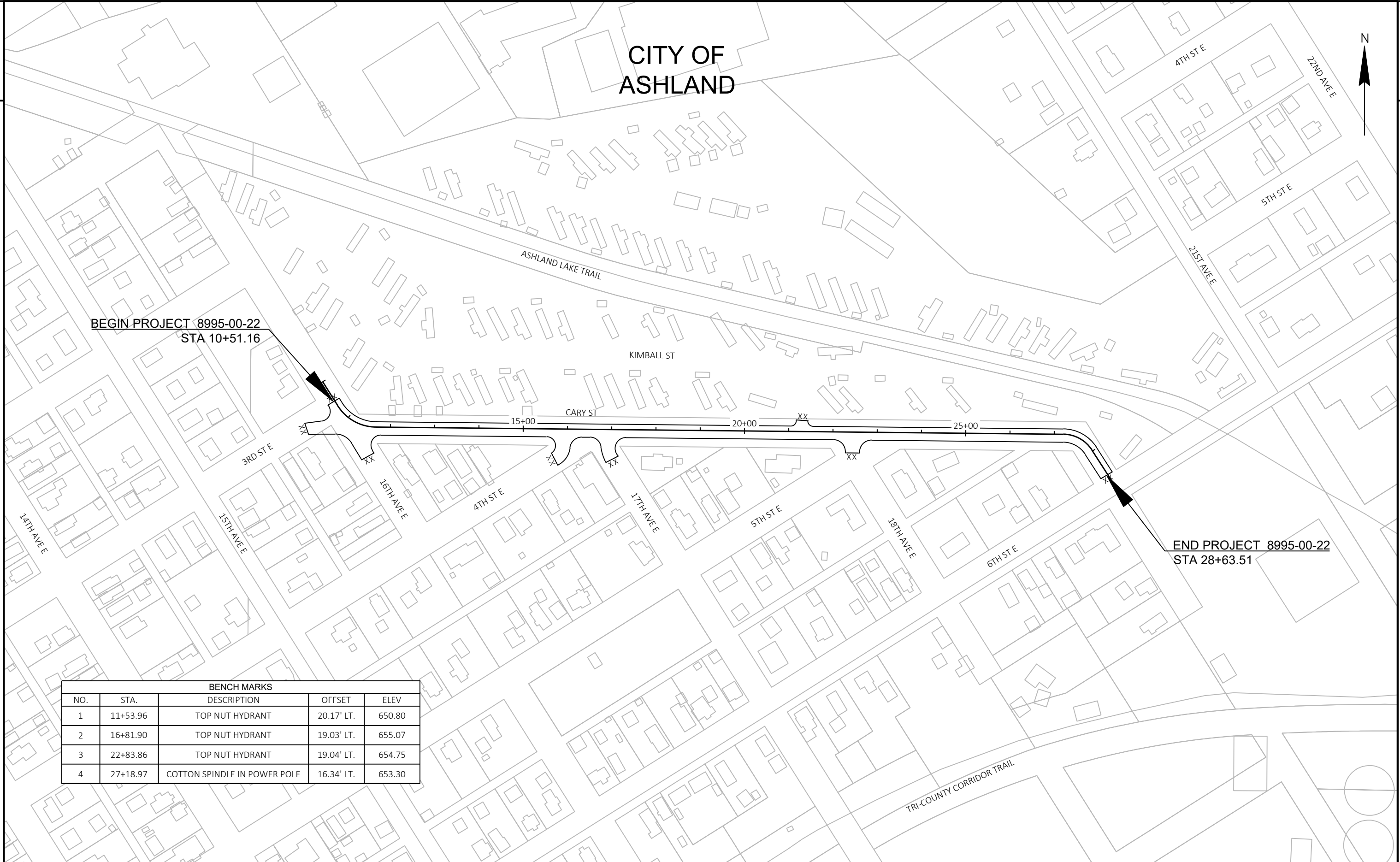
DIGGERS HOTLINE logo with a shovel icon. Text: Dial 811 or (800)242-8511. www.DiggersHotline.com

RUNOFF COEFFICIENT TABLE

Table with columns: LAND USE, HYDROLOGIC SOIL GROUP (A, B, C, D), and sub-columns for slope ranges (0-2, 2-6, 6 & OVER). Rows include ROW CROPS, MEDIAN STRIPTURF, SIDE SLOPETURF, PAVEMENT (ASPHALT, CONCRETE, BRICK, DRIVES, WALKS, ROOFS, GRAVEL ROADS, SHOULDERS).

TOTAL PROJECT AREA = 1.51 ACRES
TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = 0.83 ACRES

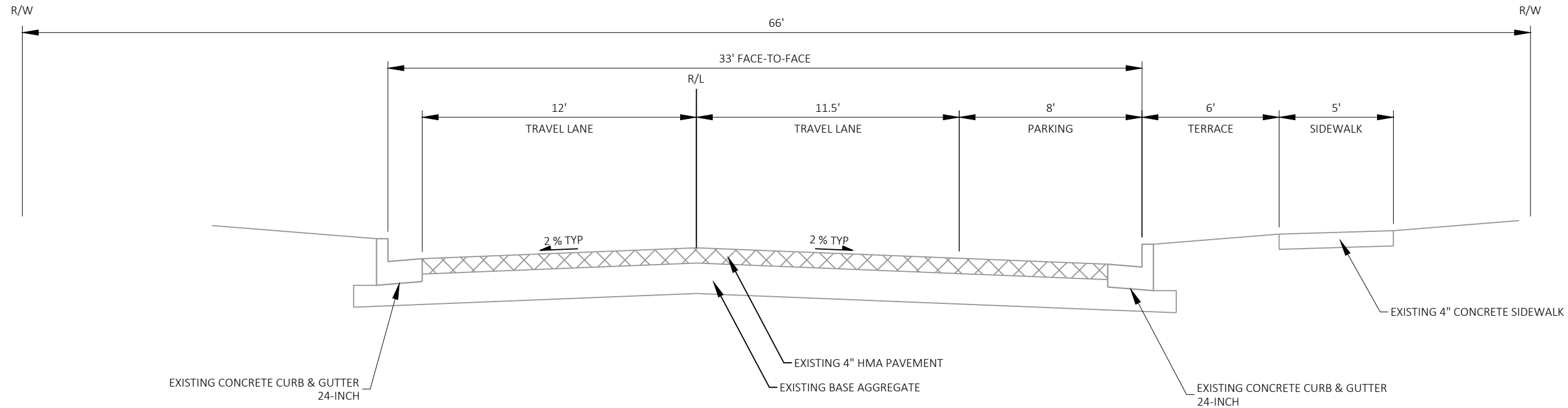
CITY OF ASHLAND



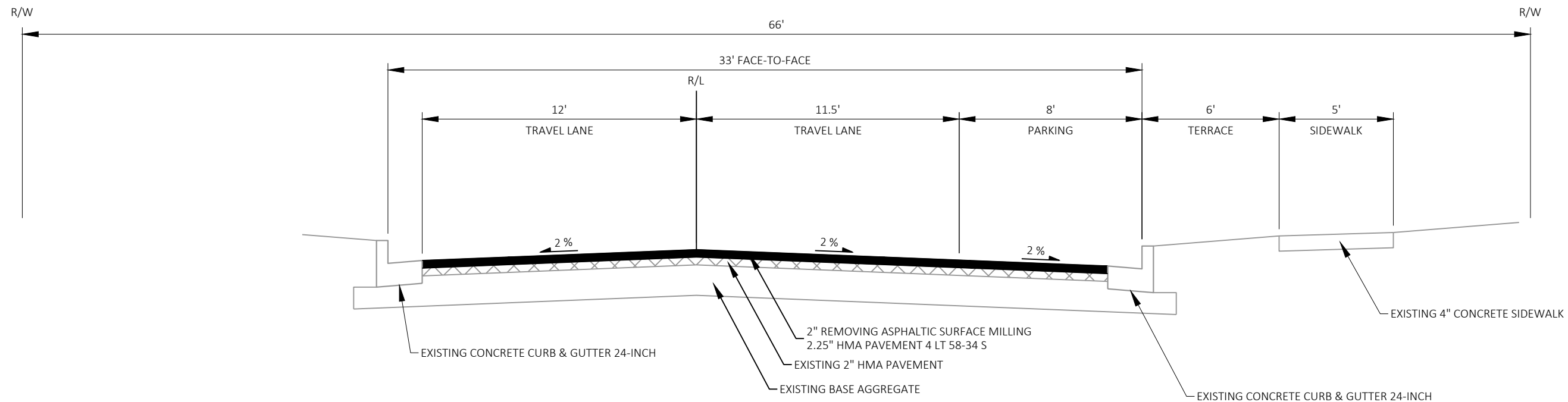
BEGIN PROJECT 8995-00-22
STA 10+51.16

END PROJECT 8995-00-22
STA 28+63.51

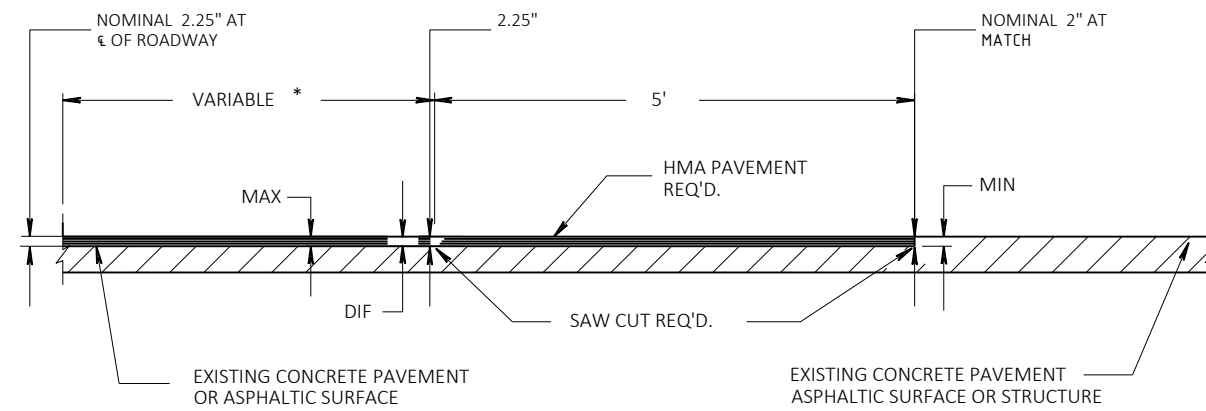
BENCH MARKS				
NO.	STA.	DESCRIPTION	OFFSET	ELEV
1	11+53.96	TOP NUT HYDRANT	20.17' LT.	650.80
2	16+81.90	TOP NUT HYDRANT	19.03' LT.	655.07
3	22+83.86	TOP NUT HYDRANT	19.04' LT.	654.75
4	27+18.97	COTTON SPINDLE IN POWER POLE	16.34' LT.	653.30



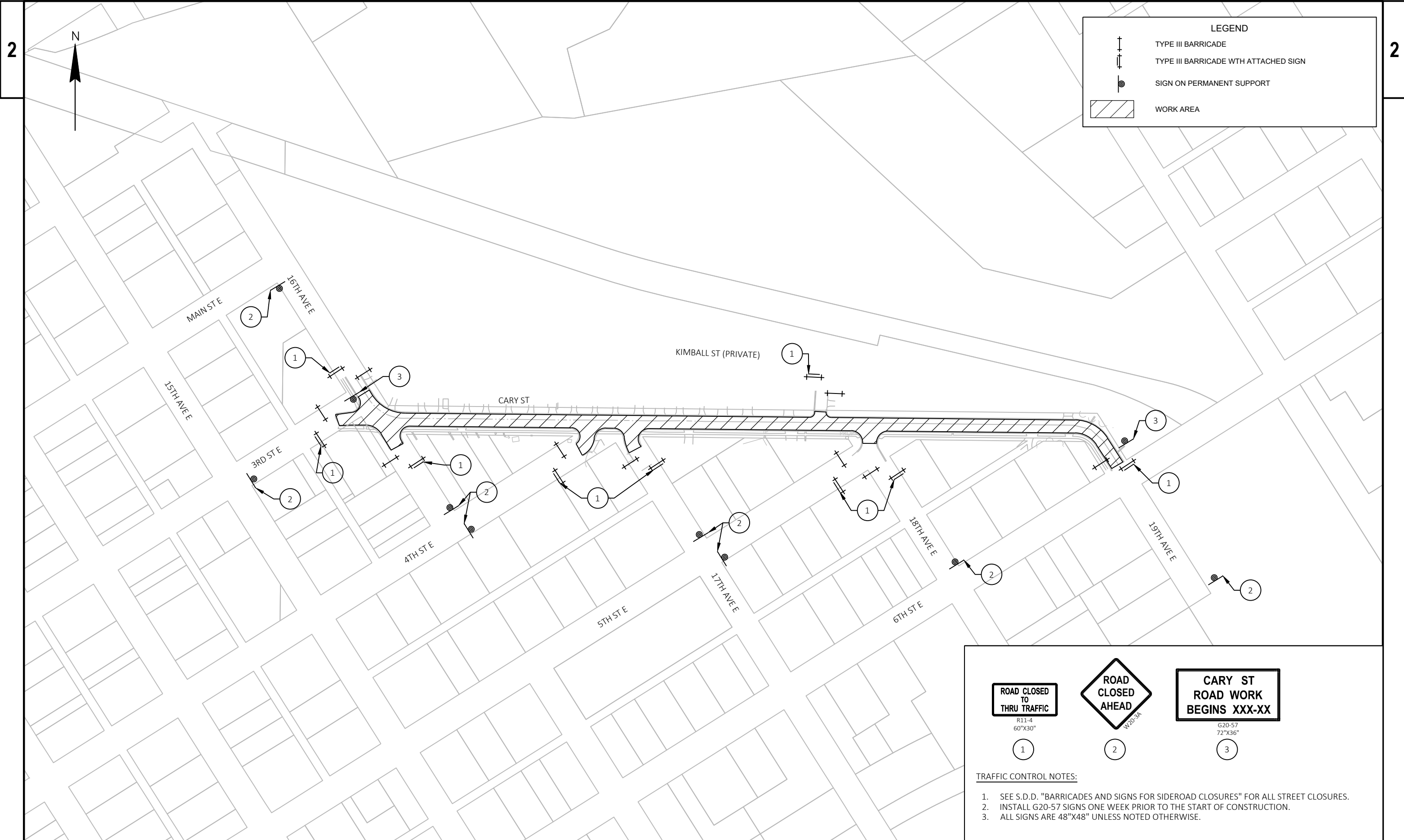
EXISTING TYPICAL SECTION - CARY ST
STA 10+51 - 28+64



FINISHED TYPICAL SECTION - CARY ST
STA 10+51 - 28+64



DETAIL OF BUTTED JOINT
 * EXACT DIMENSIONS TO BE DETERMINED BY ENGINEER IN THE FIELD.



PROJECT NO: 8995-00-22

HWY: CARY STREET

COUNTY: ASHLAND

TRAFFIC CONTROL

SHEET

E

FILE NAME : 025100-TC.DWG
LAYOUT NAME - TC-01

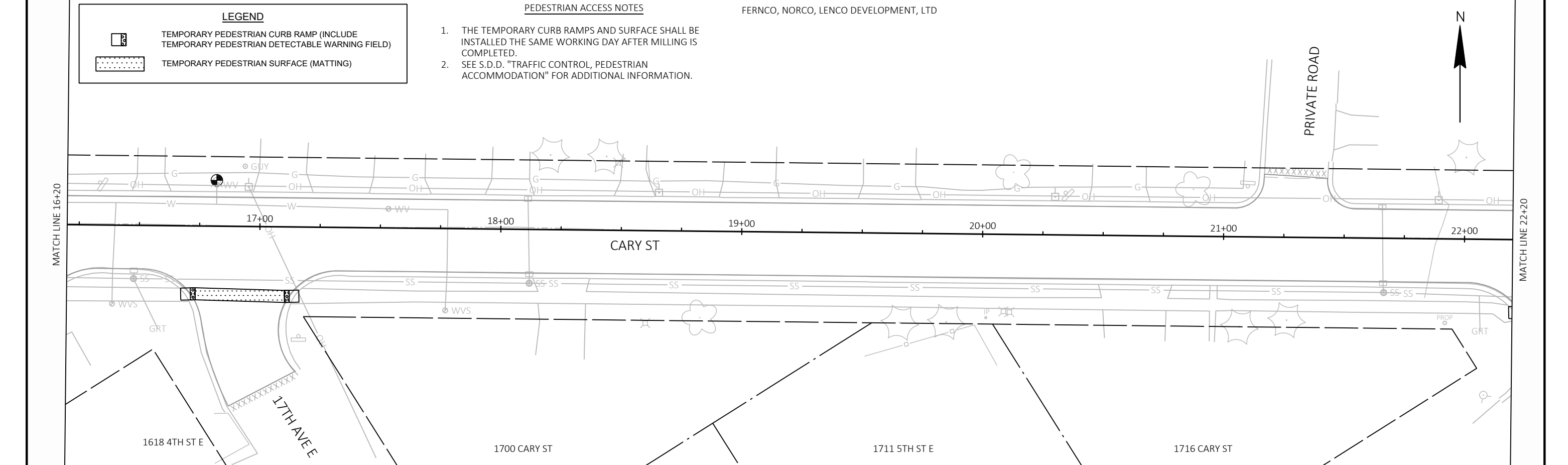
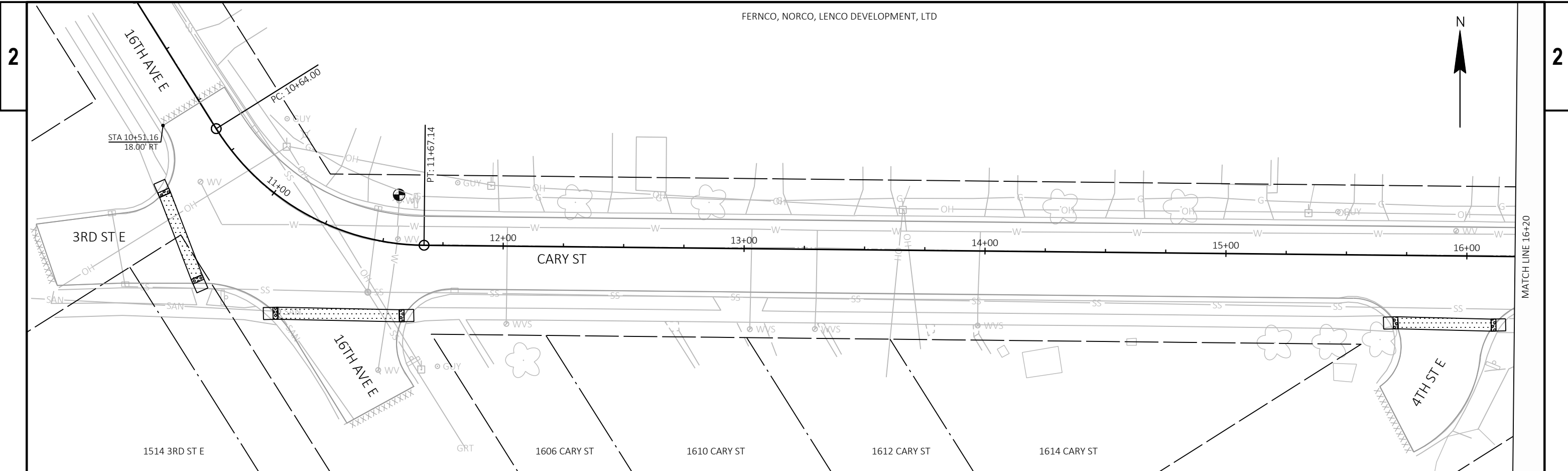
PLOT DATE : 1/31/2024 12:19 PM

PLOT BY : JORDAN DISTERHAFT

PLOT NAME :

PLOT SCALE : 1 IN:200 FT

WISDOT/CADD SHEET 42



LEGEND

- TEMPORARY PEDESTRIAN CURB RAMP (INCLUDE TEMPORARY PEDESTRIAN DETECTABLE WARNING FIELD)
- TEMPORARY PEDESTRIAN SURFACE (MATTING)

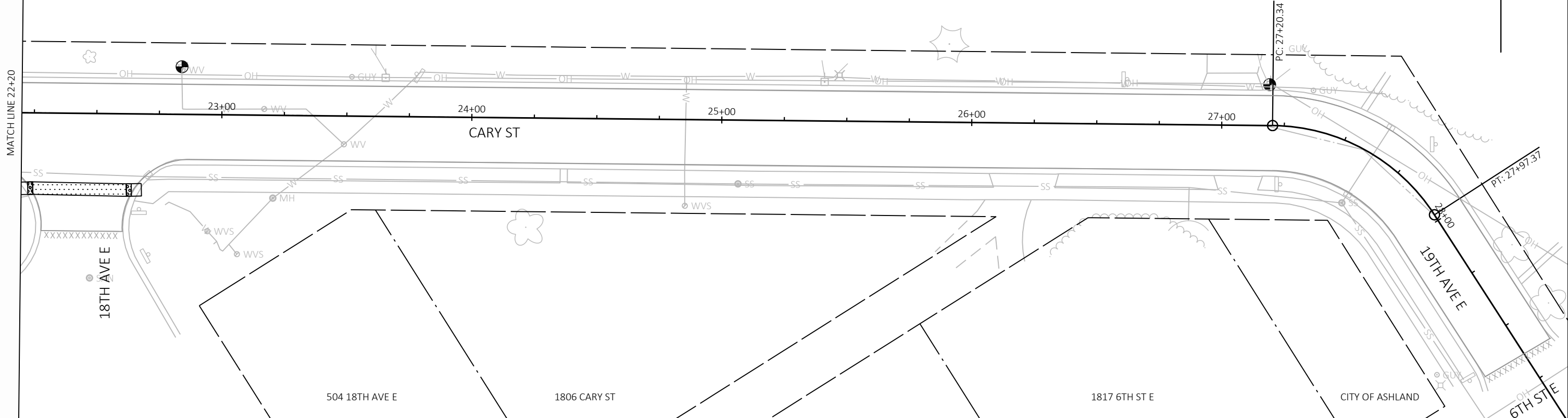
PEDESTRIAN ACCESS NOTES

1. THE TEMPORARY CURB RAMPS AND SURFACE SHALL BE INSTALLED THE SAME WORKING DAY AFTER MILLING IS COMPLETED.
2. SEE S.D.D. "TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION" FOR ADDITIONAL INFORMATION.


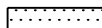


2

2



LEGEND

-  TEMPORARY PEDESTRIAN CURB RAMP (INCLUDE TEMPORARY PEDESTRIAN DETECTABLE WARNING FIELD)
-  TEMPORARY PEDESTRIAN SURFACE (MATTING)

PEDESTRIAN ACCESS NOTES

1. THE TEMPORARY CURB RAMPS AND SURFACE SHALL BE INSTALLED THE SAME WORKING DAY AFTER MILLING IS COMPLETED.
2. SEE S.D.D. "TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION" FOR ADDITIONAL INFORMATION.

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Estimate Of Quantities By Plan Sets

8995-00-22

Line	Item	Item Description	Unit	Total	Qty
0006	204.0115	Removing Asphaltic Surface Butt Joints	SY	134.000	134.000
0008	204.0120	Removing Asphaltic Surface Milling	SY	7,290.000	7,290.000
0012	213.0100	Finishing Roadway (project) 01. 8995-00-22	EACH	1.000	1.000
0020	455.0605	Tack Coat	GAL	510.000	510.000
0022	460.2000	Incentive Density HMA Pavement	DOL	590.000	590.000
0024	460.5244	HMA Pavement 4 LT 58-34 S	TON	918.000	918.000
0036	618.0100	Maintenance and Repair of Haul Roads (project) 01. 8995-00-22	EACH	1.000	1.000
0040	619.1000	Mobilization	EACH	0.350	0.350
0044	628.1905	Mobilizations Erosion Control	EACH	1.000	1.000
0046	628.1910	Mobilizations Emergency Erosion Control	EACH	1.000	1.000
0050	628.7015	Inlet Protection Type C	EACH	13.000	13.000
0054	642.5001	Field Office Type B	EACH	0.500	0.500
0058	643.0420	Traffic Control Barricades Type III	DAY	630.000	630.000
0060	643.0705	Traffic Control Warning Lights Type A	DAY	1,260.000	1,260.000
0062	643.0900	Traffic Control Signs	DAY	700.000	700.000
0064	643.1000	Traffic Control Signs Fixed Message	SF	36.000	36.000
0066	643.5000	Traffic Control	EACH	0.500	0.500
0068	644.1440	Temporary Pedestrian Surface Matting	SF	1,010.000	1,010.000
0070	644.1601	Temporary Pedestrian Curb Ramp	DAY	350.000	350.000
0072	644.1605	Temporary Pedestrian Detectable Warning Field	SF	100.000	100.000
0074	650.8000	Construction Staking Resurfacing Reference	LF	1,813.000	1,813.000
0076	650.9911	Construction Staking Supplemental Control (project) 01. 8995-00-22	EACH	1.000	1.000
0084	740.0440	Incentive IRI Ride	DOL	1,380.000	1,380.000
0090	SPV.0060	Special 01. Adjusting Water Valve Boxes	EACH	4.000	4.000
0092	SPV.0060	Special 02. Replacing Water Valve Boxes	EACH	3.000	3.000

PAVEMENT REMOVAL

CATEGORY	STATION	TO	STATION	LOCATION	204.0115	204.0120
					REMOVING ASPHALTIC SURFACE JOINTS SY	REMOVING ASPHALTIC SURFACE MILLING SY
0010	10+51	-	28+64	CARY ST	-	7,290
0010	10+51			CARY ST	17	-
0010	10+58	-	10+75	3RD ST E	15	-
0010	11+49	-	11+65	16TH AVE E	18	-
0010	15+65	-	15+79	4TH ST E	17	-
0010	16+88	-	17+15	17TH AVE E	18	-
0010	21+17	-	21+43	KIMBALL ST	14	-
0010	22+28	-	22+61	18TH AVE E	18	-
0010	28+64			CARY ST	17	-
TOTAL 0010					134	7,290

EROSION CONTROL

CATEGORY	LOCATION	628.1905	628.1910	628.7015
		MOBILIZATIONS EROSION CONTROL EACH	MOBILIZATIONS EMERGENCY EROSION CONTROL EACH	INLET PROTECTION TYPE C EACH
0010	PROJECT			13
0010	PROJECT	1	1	
TOTAL 0010		1	1	13

HMA PAVING

CATEGORY	STATION	TO	STATION	LOCATION	455.0605	460.5244
					TACK COAT GAL	HMA PAVEMENT 4 LT 58-34 S TON
0010	10+51	-	28+64	CARY ST	510	918
TOTAL 0010					510	918

CONSTRUCTION STAKING

CATEGORY	STATION	TO	STATION	LOCATION	650.8000	650.9911.01
					CONSTRUCTION STAKING RESURFACING REFERENCE LF	CONSTRUCTION STAKING SUPPLEMENTAL CONTROL (PROJECT) (01. 8995-00-22) EACH
0010	10+51	-	28+64	CARY ST	1,813	
0010				PROJECT		1
TOTAL 0010					1,813	1

3

3

TRAFFIC CONTROL

CATEGORY	LOCATION	643.0420		643.0705		643.0900		643.1000		643.5000
		TRAFFIC CONTROL BARRICADES TYPE III QTY	DAY	TRAFFIC CONTROL WARNING LIGHTS TYPE A QTY	DAY	TRAFFIC CONTROL SIGNS QTY	DAY	TRAFFIC CONTROL SIGNS FIXED MESSAGE SF	TRAFFIC CONTROL EACH	
0010	CARY ST 7-DAY ADVANCED WARNING PROJECT	18	630	36	1,260	20	700	36		
	TOTAL 0010		630		1,260		700	36		0.5

PEDESTRIAN ACCESS

CATEGORY	STATION	TO	STATION	OFFSET	LOCATION	644.1440	644.1601	644.1605
						TEMPORARY PEDESTRIAN SURFACE MATTING SF	TEMPORARY PEDESTRIAN CURB RAMP DAY	TEMPORARY PEDESTRIAN DETECTABLE WARNING FIELD SF
0010	10+69	-	11+00	RT	CARY ST/3RD ST E	185	70	20
	11+20	-	11+65	RT	CARY ST/16TH AVE E	255	70	20
	15+66	-	16+16	RT	CARY ST/4TH ST E	195	70	20
	16+67	-	17+16	RT	CARY ST/17TH AVE E	185	70	20
	22+19	-	22+68	RT	CARY ST/18TH AVE E	190	70	20
					TOTAL 0010	1,010	350	100

3

MOBILIZATION

CATEGORY	LOCATION	619.1000 MOBILIZATION EACH
0010	PROJECT	0.35
	TOTAL 0010	0.35

FIELD OFFICE

CATEGORY	LOCATION	642.5001 FIELD OFFICE TYPE B EACH
0010	PROJECT	0.5
	TOTAL 0010	0.5

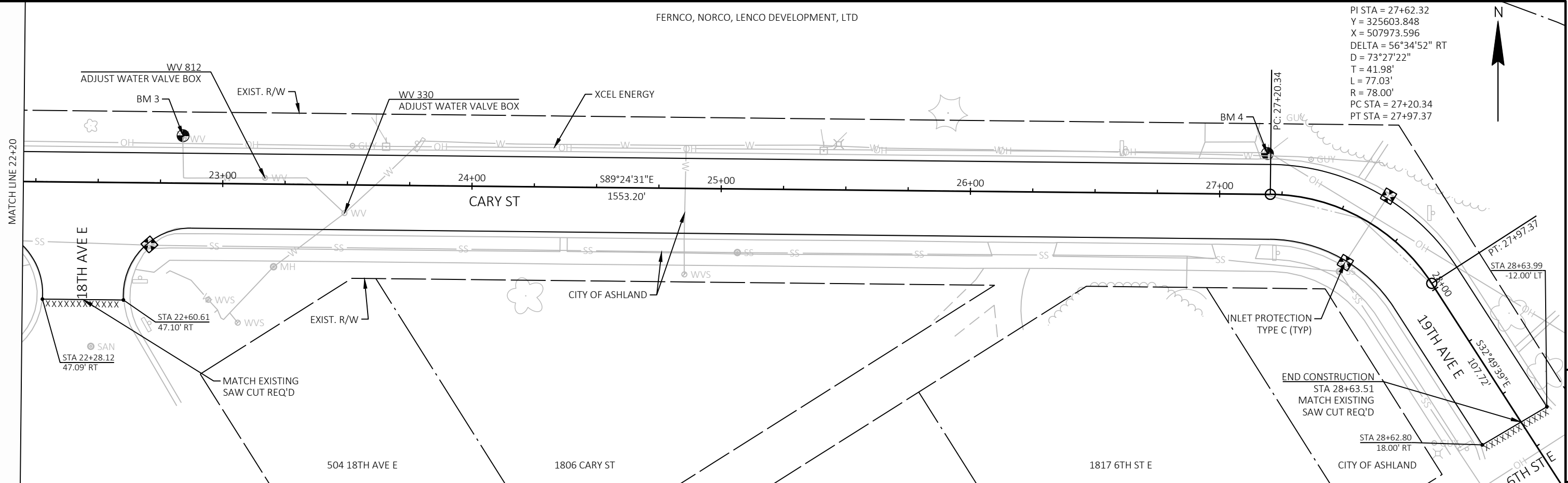
CATEGORY 20

618.0100.01 MAINTENANCE AND REPAIR OF HAUL ROADS (PROJECT) (01. 8995-00-22)	SPV.0060.01 SPECIAL (01. ADJUSTING WATER VALVE BOXES)	SPV.0060.02 SPECIAL (02. REPLACING WATER VALVE BOXES)
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CATEGORY	STATION	OFFSET	LOCATION	EACH	EACH	EACH	REMARKS
0020	10+77	RT				1	WV 326
	11+23	RT					SAN 450
	11+55	RT			1		WV 1258
	11+56	LT				1	WV 327
	15+95	LT			1		WV 329
	17+53	LT				1	WV328
	23+14	LT			1		WV812
	23+49	RT			1		WV 330
				PROJECT	1		
			TOTAL 0020	1	4	3	

3

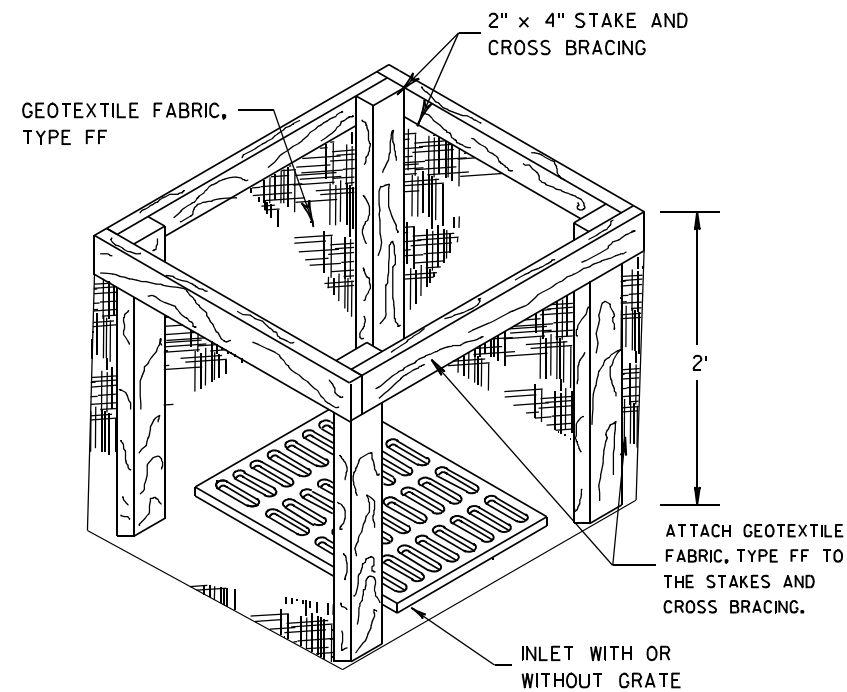
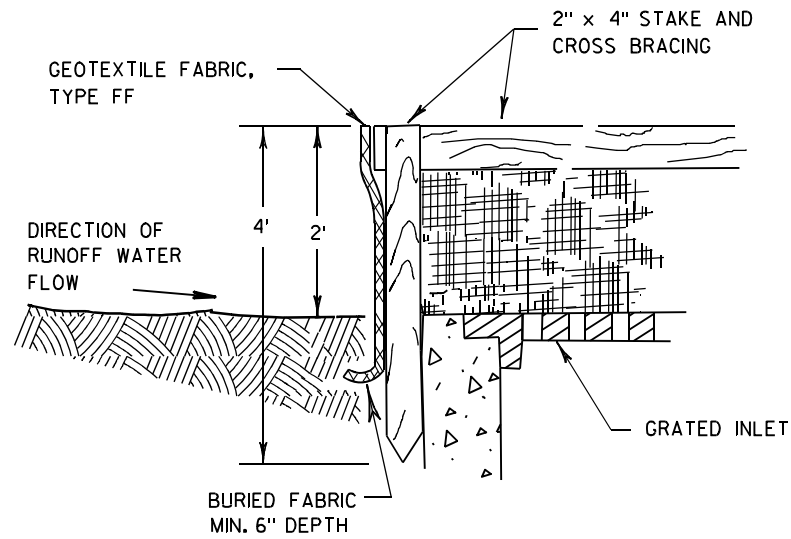
PI STA = 27+62.32
Y = 325603.848
X = 507973.596
DELTA = 56°34'52" RT
D = 73°27'22"
T = 41.98'
L = 77.03'
R = 78.00'
PC STA = 27+20.34
PT STA = 27+97.37



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Standard Detail Drawing List

08E10-02	INLET PROTECTION TYPE A, B, C AND D
15C02-09A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-09B	BARRICADES AND SIGNS FOR VARIOUS CLOSURES
15C03-05	BARRICADES AND SIGNS FOR SIDEROAD CLOSURES
15C05-05	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M.P.H. OR LESS
15D30-09C	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION
15D30-09L	TRAFFIC CONTROL, PEDESTRIAN ACCOMMODATION



INLET PROTECTION, TYPE A

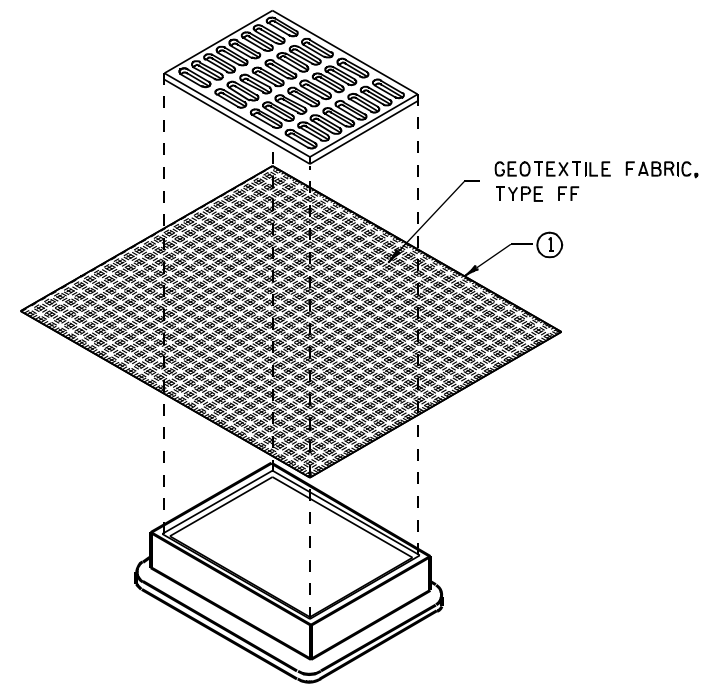
GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

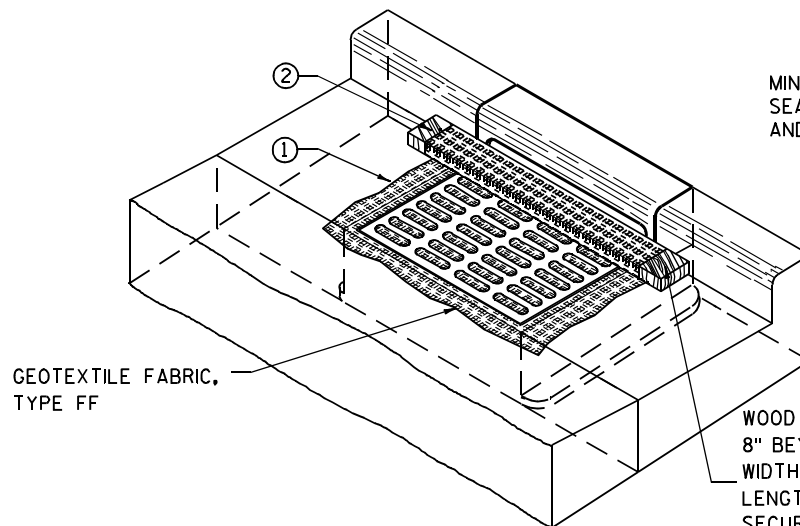
WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- ① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- ② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- ③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



**INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)**

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE C (WITH CURB BOX)

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

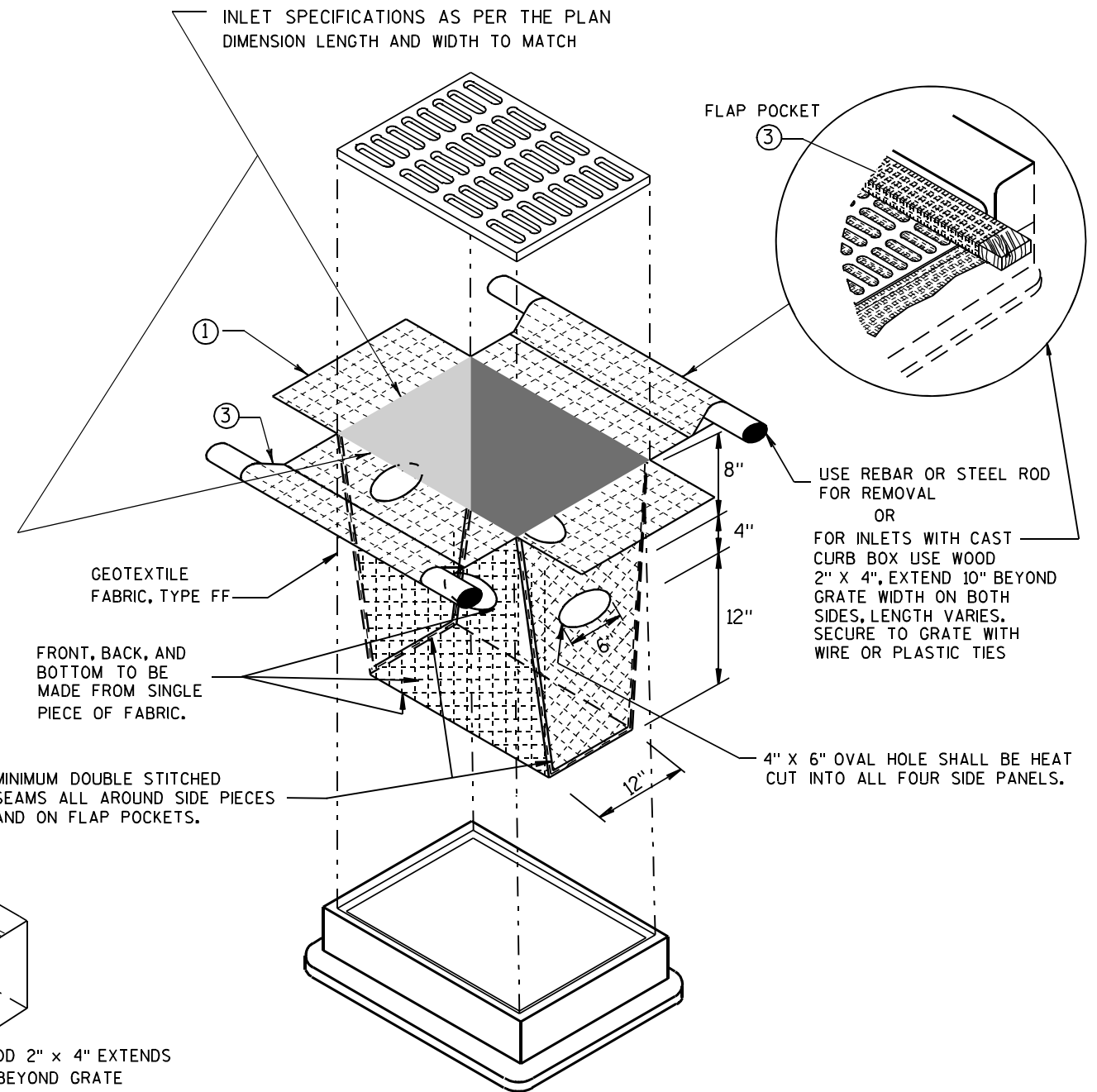
THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

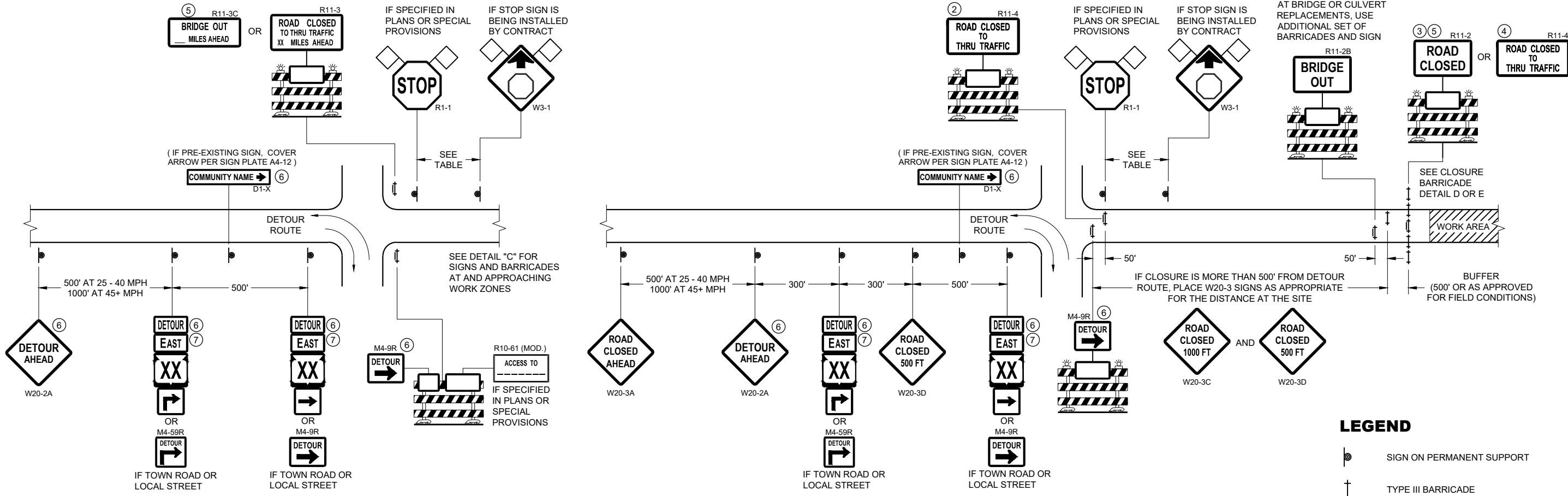
THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.



INLET PROTECTION, TYPE D

(CAN BE INSTALLED IN ANY INLET TYPE WITH OR WITHOUT A CURB BOX AS PER NOTE ②)

INLET PROTECTION TYPE A, B, C, AND D	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 10/16/02 DATE	/S/ Beth Connestra CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA	



**DETAIL A
MAINLINE CLOSURE WITH POSTED DETOUR**

WORK ZONE GREATER THAN OR EQUAL TO 1/2 MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)

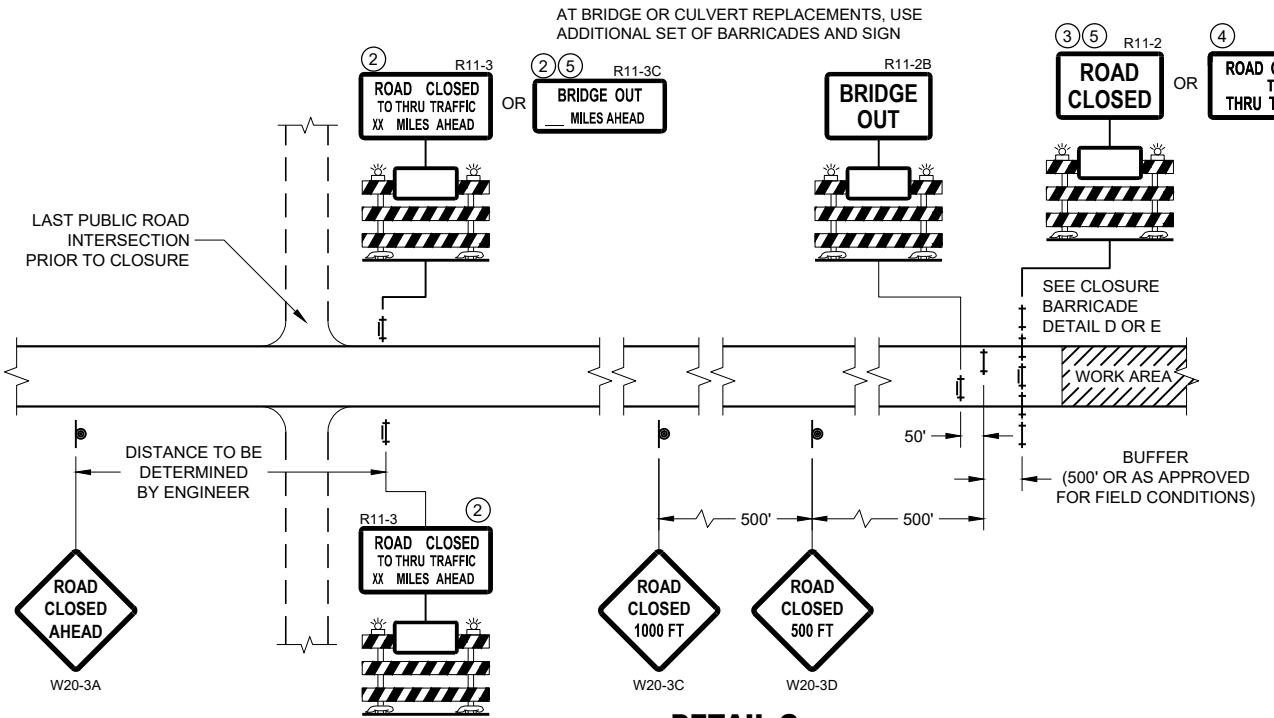
**DETAIL B
MAINLINE CLOSURE WITH POSTED DETOUR**

WORK ZONE LESS THAN 1/2 MILE FROM
DETOUR ROUTE (1000 FEET IF URBAN)

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA
- FLAGS, 16" X 16" MIN. (ORANGE)
- M4 - 8
- M3 - X
- M1 - 4 OR M1 - 6 OR M1 - 5A
- M05 - 1 OR M06 - 1

SPEED LIMIT (MPH)	"STOP AHEAD" ADVANCE WARNING DISTANCE (FT)
25	200
30	200
35	350
40	350
45	500
50	550
55	750



**DETAIL C
MAINLINE CLOSURE, NO POSTED DETOUR**

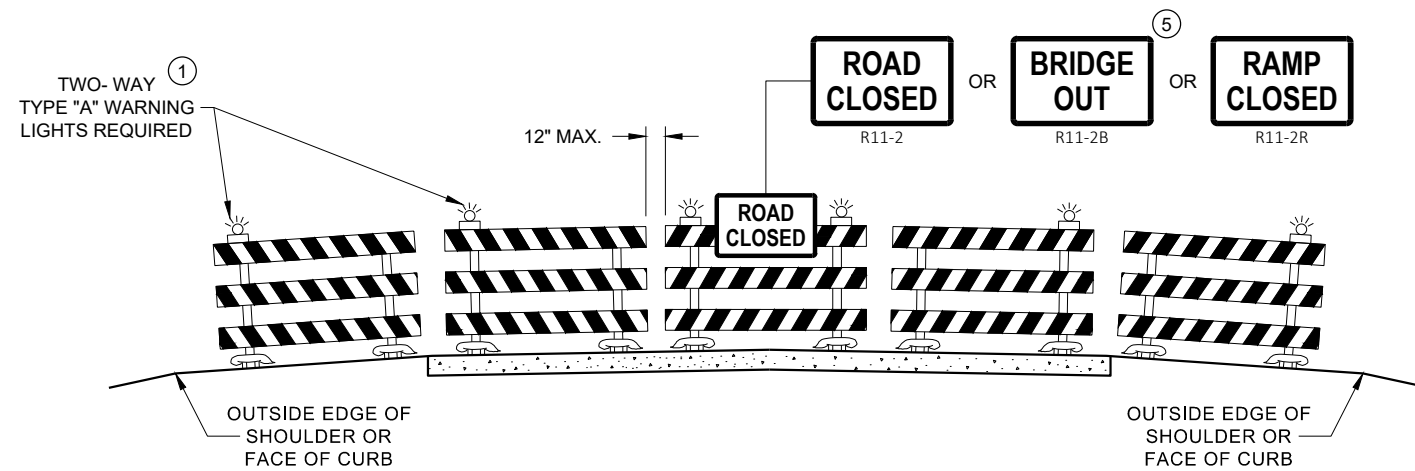
SEE SDD 15C2-SHEET "b"
FOR GENERAL NOTES
AND FOOTNOTES ① THROUGH ⑦

**BARRICADES AND SIGNS
FOR MAINLINE CLOSURES**

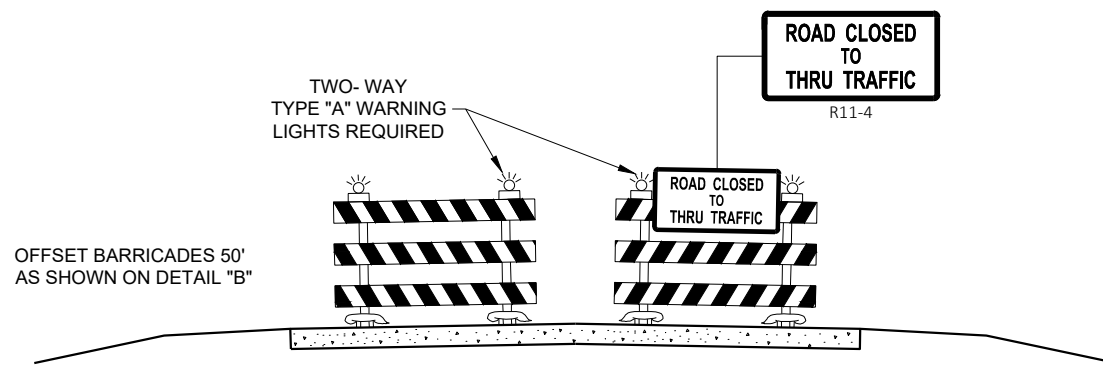
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 /S/ Andrew Heidtke
DATE DATE WORK ZONE ENGINEER

FHWA



**DETAIL D
ROAD CLOSURE BARRICADE DETAIL
APPROACH VIEW**



**DETAIL E
LANE CLOSURE BARRICADE DETAIL
APPROACH VIEW**

SEE SDD 15C2 - SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW - INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11 - 2, R11 - 3, M4 - 9, R11 - 4, AND R10 - 61 SIGNS PLACED ON THE BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE RAIL OR BOTTOM RAILS.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

- R11 - 2 SHALL BE 48" X 30"
- R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60" X 30"
- M4 - 9 SHALL BE 30" X 24"
- M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1 - 1 SHALL BE 36" X 36"

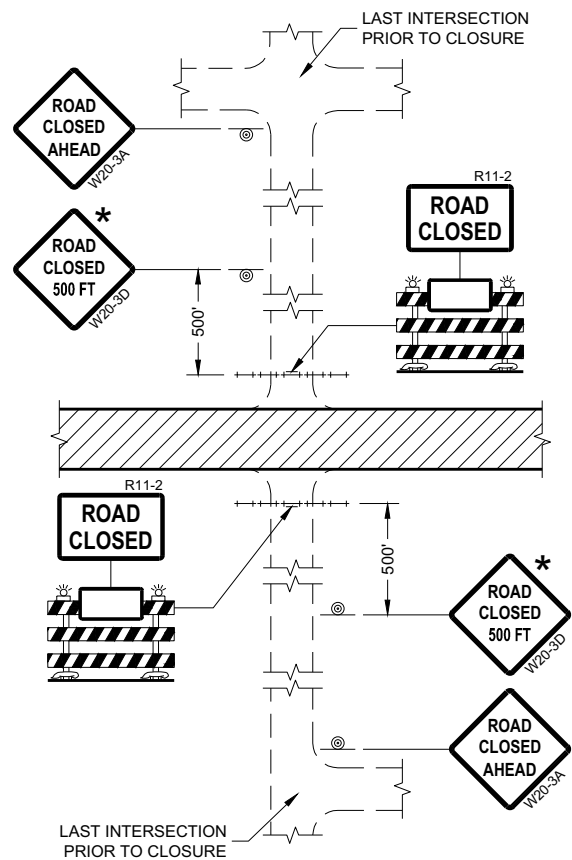
- ① TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT SPACING).
- ② THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.
- ③ FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".
- ④ FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".
- ⑤ FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.
- ⑥ INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.
- ⑦ "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

**BARRICADES AND SIGNS
FOR
VARIOUS CLOSURES**

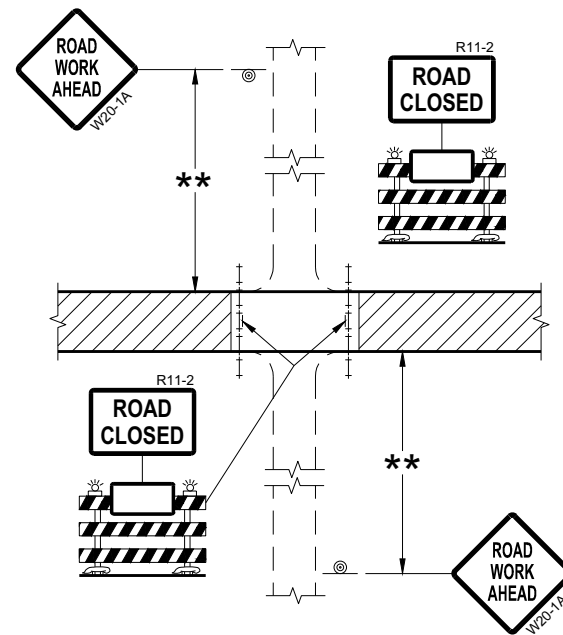
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
May 2023 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

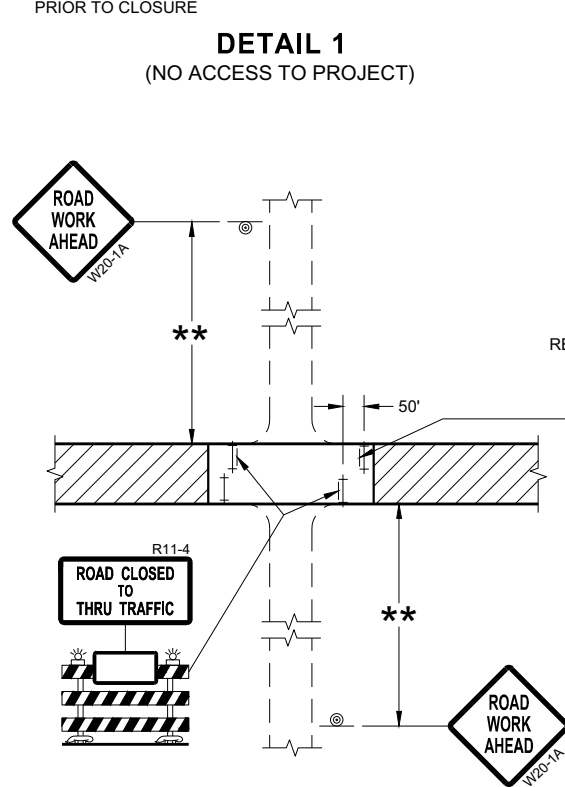
FHWA



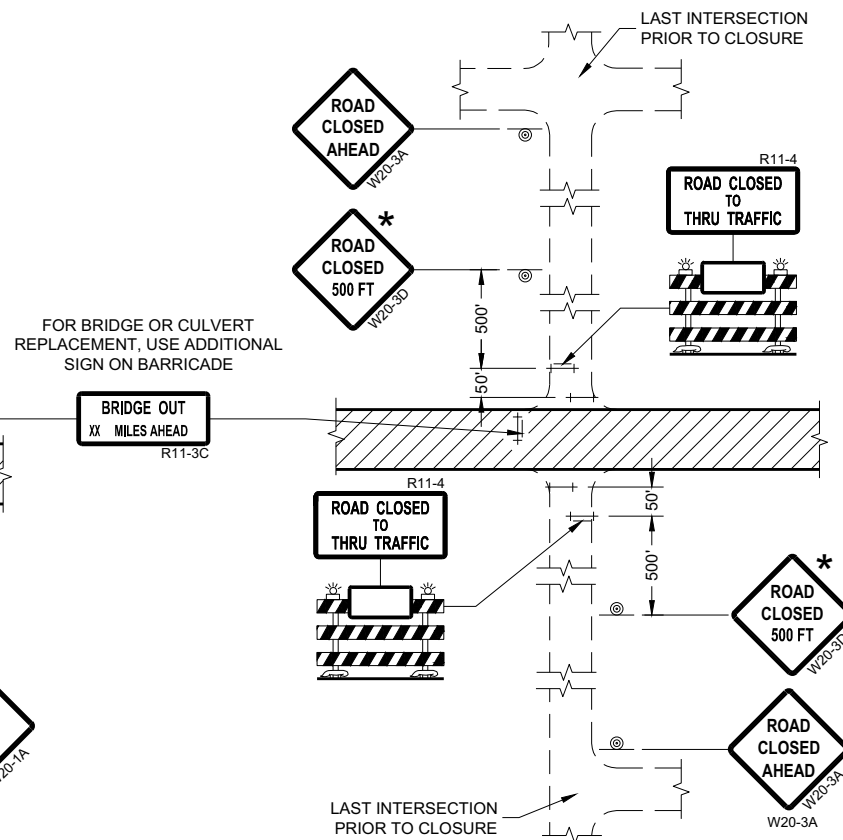
DETAIL 1
(NO ACCESS TO PROJECT)



DETAIL 2
(PUBLIC CROSS-TRAFFIC MAINTAINED.
NO ACCESS TO PROJECT)



DETAIL 3
(PUBLIC CROSS-TRAFFIC MAINTAINED.
CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)



DETAIL 4
(CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE (500 FEET DESIRABLE) TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3, AND R11-4 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
R11-2 SHALL BE 48" X 30".
R11-4 AND R11-3 SHALL BE 60" X 30".

* OMIT THE "ROAD CLOSED 500 FT." SIGN IF THE LAST INTERSECTION IS 500 FEET OR LESS FROM THE WORK ZONE.

** 500' MAX. OR AT LAST INTERSECTION, WHICHEVER IS CLOSEST.

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA

**BARRICADES AND SIGNS
FOR
SIDEROAD CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
July 2018 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.




ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS, 36"X36" SIGNS MAY BE USED INSTEAD OF 48" X 48" SIGNS.

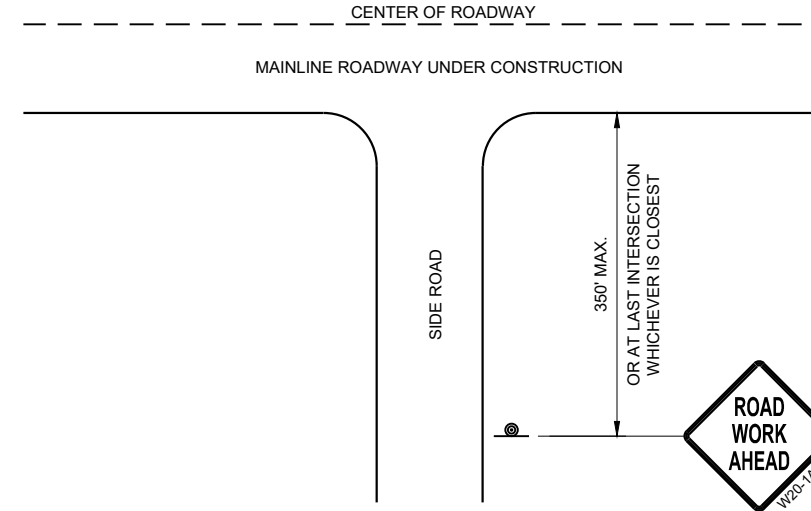
SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

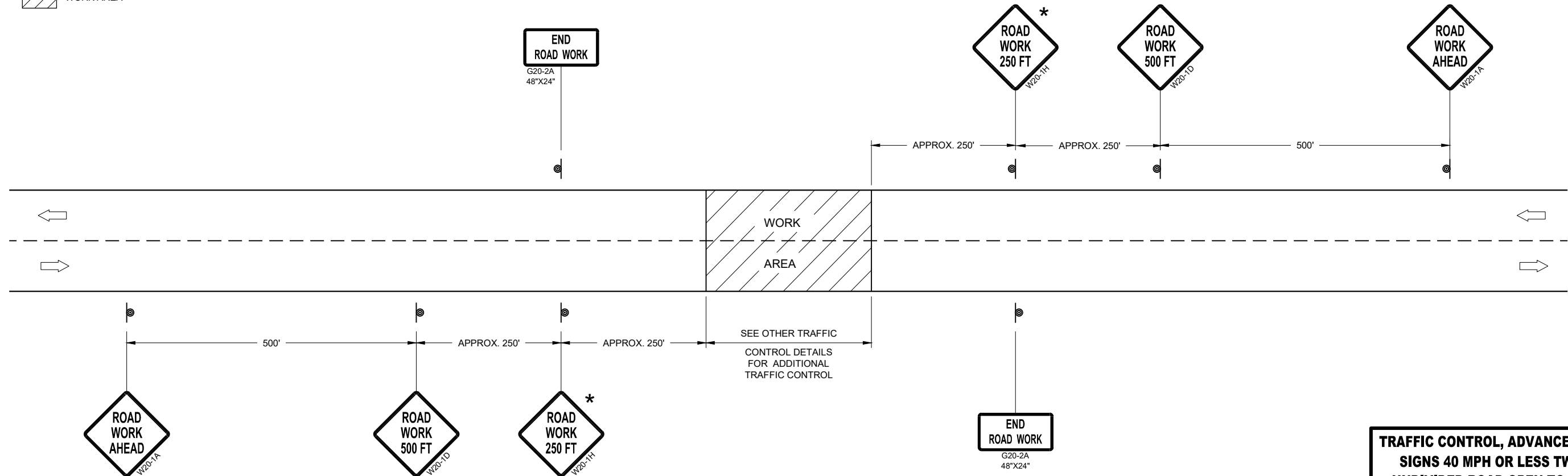
* THE THIRD W20-1 SIGN IS REQUIRED ONLY IF THERE IS AN INTERSECTION BETWEEN THE "ROAD WORK 500 FEET" SIGN AND THE WORK ZONE. ADJUST THE PLACEMENT OF THIS SIGN BASED ON INTERSECTION LOCATION AND OTHER FIELD CONDITIONS.

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC
-  WORK AREA



TYPICAL SIDE ROAD APPROACH WARNING SIGN DETAIL



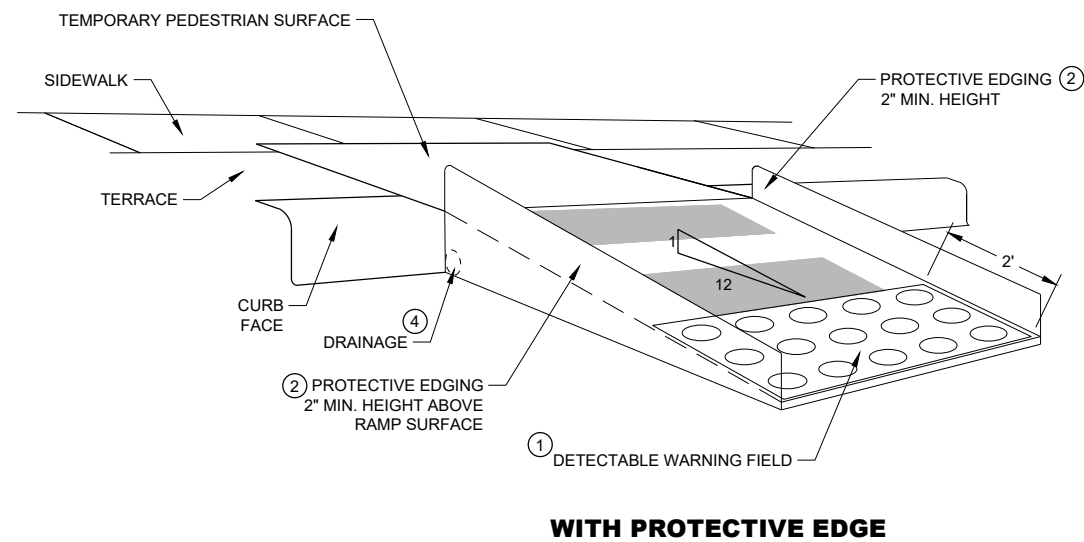
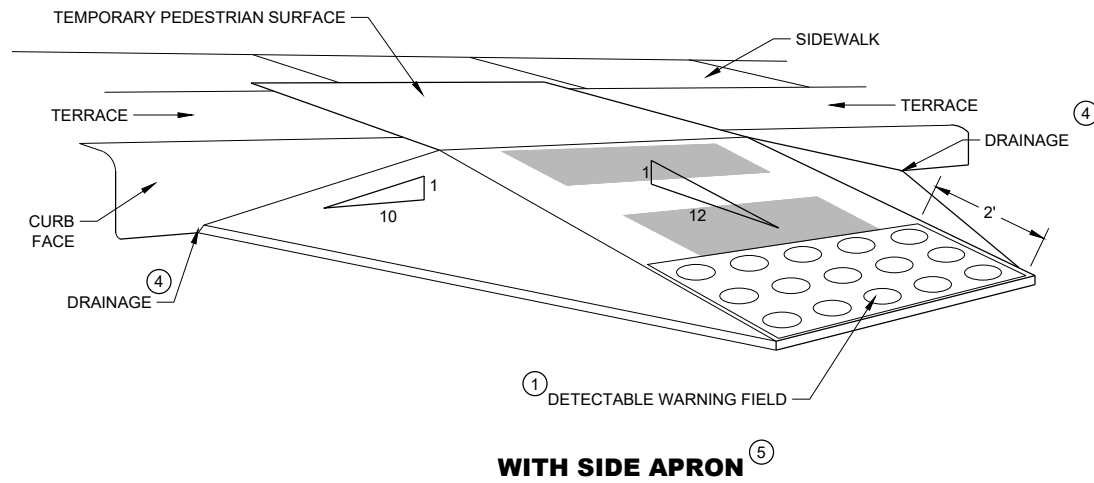
TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40MPH OR LESS

TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 MPH OR LESS TWO-WAY UNDIVIDED ROAD OPEN TO TRAFFIC

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE July 2018 /S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA



TEMPORARY CURB RAMP PERPENDICULAR TO CURB

GENERAL NOTES

CURB RAMPS SHALL BE 48" MINIMUM WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE.

ALTERNATE SIDEWALK WORK BETWEEN LEFT AND RIGHT SIDE OF ROADWAY TO MAINTAIN PEDESTRIAN ACCESS.

CURB RAMPS AND LANDINGS SHALL HAVE A 1:50 (2%) MAX. CROSS-SLOPE.

CLEAR SPACE OF 48" X 48" SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.

LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.

CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES MAY BE VERTICAL UP TO 1/4" HIGH AND SHALL BE BEVELED AT 1:2 BETWEEN 1/4" AND 1/2".

- ① INSTALL CONTRASTING TEMPORARY DETECTABLE WARNING FIELD AT PEDESTRIAN STREET CROSSINGS, AS SHOWN IN THE PLANS
- ② PROTECTIVE EDGING WITH A 2" MIN. HEIGHT SHALL BE INSTALLED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- ③ DETECTABLE EDGING WITH 6" MIN. HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- ④ DO NOT RESTRICT WATER FLOW IN THE GUTTER SYSTEM.
- ⑤ CAN ONLY BE USED FOR RAMPS WITH 6" OR LESS OF VERTICAL CHANGE.

Notes



Wisconsin Department of Transportation

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SUP MAY 2024

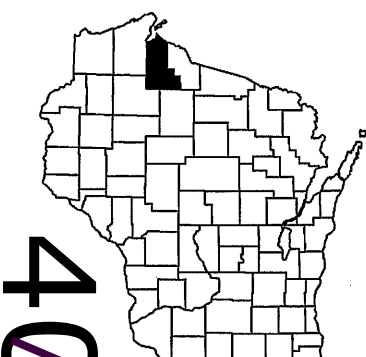
PROJECT ID: 8995-00-24
WITH: N/A

COUNTY: ASHLAND

ORDER OF SHEETS

Section No.	1	Title
Section No.	2	Typical Sections and Details
Section No.	3	Estimate of Quantities
Section No.	3	Miscellaneous Quantities
Section No.	4	Right of Way Plat
Section No.	5	Plan and Profile
Section No.	6	Standard Detail Drawings
Section No.	7	Sign Plates
Section No.	8	Structure Plans
Section No.	9	Computer Earthwork Data
Section No.	0	Cross Sections

TOTAL SHEETS = 26



DESIGN DESIGNATION

A.A.D.T.	2024	=	520
A.A.D.T.	2044	=	580
D.H.V.		=	70
D.D.		=	60/40
T.		=	20%
DESIGN SPEED		=	30 MPH
ESALS		=	N/A

CONVENTIONAL SYMBOLS

PLAN	PROFILE
CORPORATE LIMITS	GRADE LINE
PROPERTY LINE	ORIGINAL GROUND
LOT LINE	MARSH OR ROCK PROFILE (To be noted as such)
LIMITED HIGHWAY EASEMENT	SPECIAL DITCH
EXISTING RIGHT OF WAY	GRADE ELEVATION
PROPOSED OR NEW R/W LINE	CULVERT (Profile View)
SLOPE INTERCEPT	UTILITIES
REFERENCE LINE	ELECTRIC
EXISTING CULVERT	FIBER OPTIC
PROPOSED CULVERT (Box or Pipe)	GAS
COMBUSTIBLE FLUIDS	SANITARY SEWER
MARSH AREA	STORM SEWER
WOODED OR SHRUB AREA	TELEPHONE
	WATER
	UTILITY PEDESTAL
	POWER POLE
	TELEPHONE POLE

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

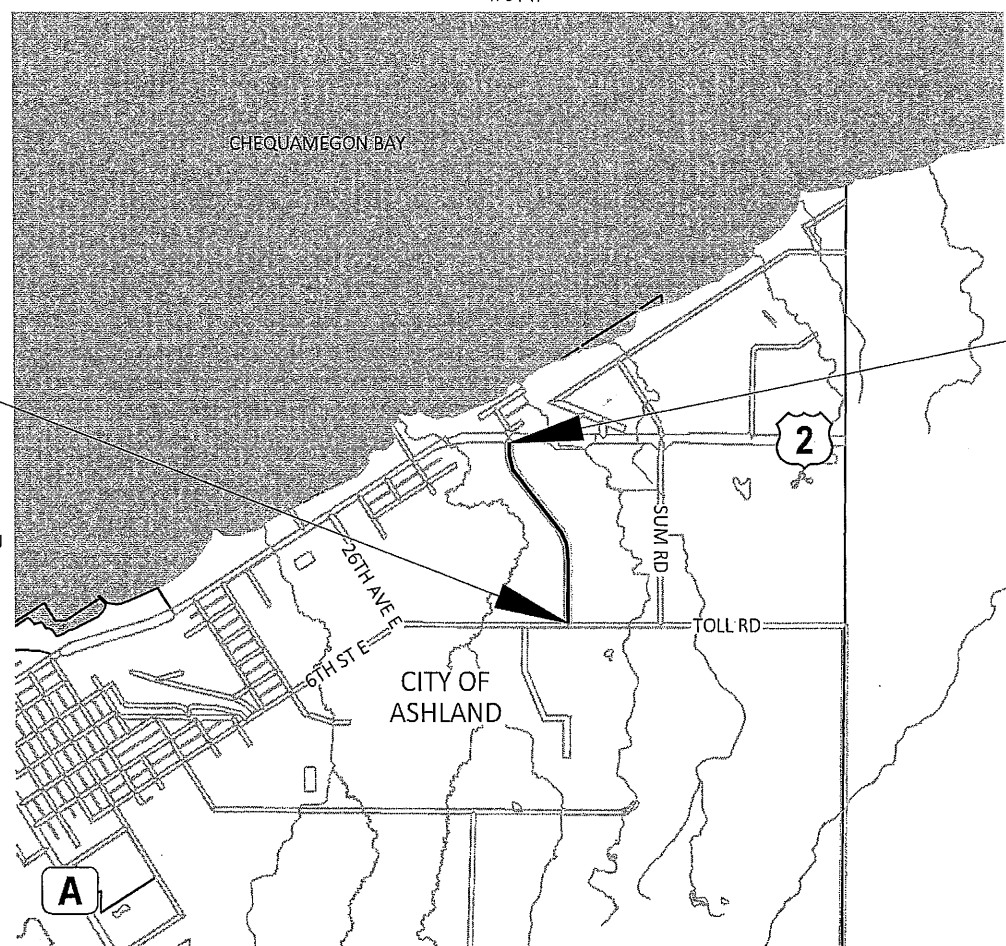
PLAN OF PROPOSED IMPROVEMENT

C ASHLAND, INDUSTRIAL PARK ROAD

TOLL RD TO USH 2
LOC STR
ASHLAND COUNTY

STATE PROJECT NUMBER
8995-00-24

STATE PROJECT	FEDERAL PROJECT	
	PROJECT	CONTRACT
8995-00-24	WISC 2024345	1



BEGIN PROJECT
STA 10+56.14
Y = 326918.97
X = 512742.37

END PROJECT
STA 38+20.46
Y = 329426.28
X = 511907.27

SCALE 0 0.5 MI
TOTAL NET LENGTH OF CENTERLINE = 0.524 MILES

HORIZONTAL POSITIONS SHOWN ON THIS PLAN ARE WISCONSIN COUNTY COORDINATE SYSTEM (WCCS), ASHLAND COUNTY, NAD83 (2011), IN U.S. SURVEY FEET. POSITIONS SHOWN ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES ARE THE SAME AS GROUND DISTANCES.
ELEVATIONS ARE REFERENCED TO NAVD 88 (2007). GPS DERIVED ELEVATIONS ARE BASED ON GEOID 18

ACCEPTED FOR
CITY OF ASHLAND
Date 4/29/24
J. H. Olson
Director
(Signature and Title of Official)

ORIGINAL PLANS PREPARED BY
Cedar corporation
MENOMONIE - MADISON - GREEN BAY - CEDARBURG
www.cedarcorp.com
WISCONSIN PROFESSIONAL ENGINEER
WILLIAM A. BETZIG
E-27689
HARTLAND, WI
DATE: 1-29-24
(Professional Engineer Signature)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION
PREPARED BY
Surveyor CEDAR CORPORATION
Designer CEDAR CORPORATION
Project Manager PAULA GROOM, P.E.
Regional Examiner TON YANG, P.E.
Regional Supervisor JEFFREY OLSON, P.E.

APPROVED FOR THE DEPARTMENT
DATE: 02/01/2024
Paula Groom
(Signature)

GENERAL NOTES

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

NO TREES OR SHRUBS ARE TO BE REMOVED UNLESS SUCH TREES OR SHRUBS HAVE FIRST BEEN INDICATED FOR REMOVAL BY THE ENGINEER IN THE FIELD.

WHEN THE QUANTITY OF ITEM BASE LAYER OR SURFACE LAYER IS MEASURED FOR PAYMENT BY THE TON, THE THICKNESS OF THE MATERIAL THAT IS SHOWN ON THE PLANS IS APPROXIMATE AND THE ACTUAL THICKNESS WILL DEPEND ON THE DISTRIBUTION OF MATERIAL AS DIRECTED BY THE ENGINEER.

STANDARD ABBREVIATIONS

Table with 4 columns: Abbreviation, Description, Abbreviation, Description. Includes terms like ABUT, AGG, ET AL, AADT, BF, BM, C/L OR e, etc.

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SPOONER, WI 54801
PHONE: (715) 416-0478
EMAIL: SHAWN.HASELEU@WISCONSIN.GOV

DESIGN CONSULTANT

CEDAR CORPORATION
DENNIS MACK, P.E.
604 WILSON AVENUE
MENOMONIE, WI 54751
PHONE: (715) 235-9081
EMAIL: DENNIS.MACK@CEDARCORP.COM

UTILITIES CONTACTS

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601 MAIN STREET W
ASHLAND, WI 54806
PHONE: (715) 685-1648
EMAIL: JBUTLER@COAWI.ORG

CITY OF ASHLAND - SEWER
JOHN BUTLER, P.E. - DIRECTOR
601 MAIN STREET W
ASHLAND, WI 54806
PHONE: (715) 685-1648
EMAIL: JBUTLER@COAWI.ORG

GAS/ELECTRIC

XCEL ENERGY
BEN KOZAK
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ASHLAND, WI 54806
PHONE: (715) 682-6915
EMAIL: BENJAMIN.KOZAK@XCELENERGY.COM

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SPECTRUM COMMUNICATIONS
RYAN NELSON
1810 LAKESHORE DRIVE EAST
ASHLAND, WI 54806
PHONE: (715) 979-5015
EMAIL: RYAN.NELSON@CHARTER.COM

COMMUNICATION

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GUY FOLSOM
43705 US HIGHWAY 63
CABLE, WI 54891
PHONE: (715) 798-7123
EMAIL: GFOLSOM@NORVADO.COM

COMMUNICATION

BRIGHTSPEED
MARK SCRIBNER
1905 WARD AVENUE
LA CROSSE, WI 54601
PHONE: (715) 492-7976
EMAIL: MARK.SCRIBNER@BRIGHTSPEED.COM

DIGGERS HOTLINE logo with a shovel icon. Text: Dial 811 or (800)242-8511 www.DiggersHotline.com

RUNOFF COEFFICIENT TABLE

Table with columns: LAND USE, HYDROLOGIC SOIL GROUP (A, B, C, D), and sub-columns for slope ranges (0-2, 2-6, 6 & OVER). Rows include ROW CROPS, MEDIAN STRIPTURF, SIDE SLOPETURF, PAVEMENT, ASPHALT, CONCRETE, BRICK, DRIVES, WALKS, ROOFS, GRAVEL ROADS, SHOULDERS.

TOTAL PROJECT AREA = 6.35 ACRES
TOTAL AREA EXPECTED TO BE DISTURBED BY CONSTRUCTION ACTIVITIES = 2.28 ACRES

CITY OF ASHLAND

END PROJECT 8995-00-24
STA 38+20.46

BEGIN PROJECT 8995-00-24
STA 10+56.14

RAIL DR

TOLL RD

BP: 10+00.00

BM 1
660.19

PC: 13+65.68

PT: 13+73.43
Mid: 13+69.56

BM 2
655.88

PC: 19+96.62

PT: 24+76.89

BM 3
654.12

Mid: 22+36.75

25+00

PI: 29+40.69

BM 4
646.27

30+00

PC: 33+57.14

BM 5
637.79

Mid: 35+54.70

PT: 37+52.25

EP: 39+29.22

USH 2 / LAKE SHORE DR

32ND AVE

INDUSTRIAL PARK RD

BENCH MARKS				
NO.	STA.	DESCRIPTION	OFFSET	ELEV
1	10+22.51	TOP NUT HYDRANT	70.99' LT.	660.19
2	19+03.89	TOP NUT HYDRANT	21.73' RT.	655.88
3	25+03.20	TOP NUT HYDRANT	21.90' RT.	654.12
4	30+83.15	TOP NUT HYDRANT	24.75' RT.	646.27
5	36+37.19	TOP NUT HYDRANT	21.87' RT.	637.79

PROJECT NO: 8995-00-24

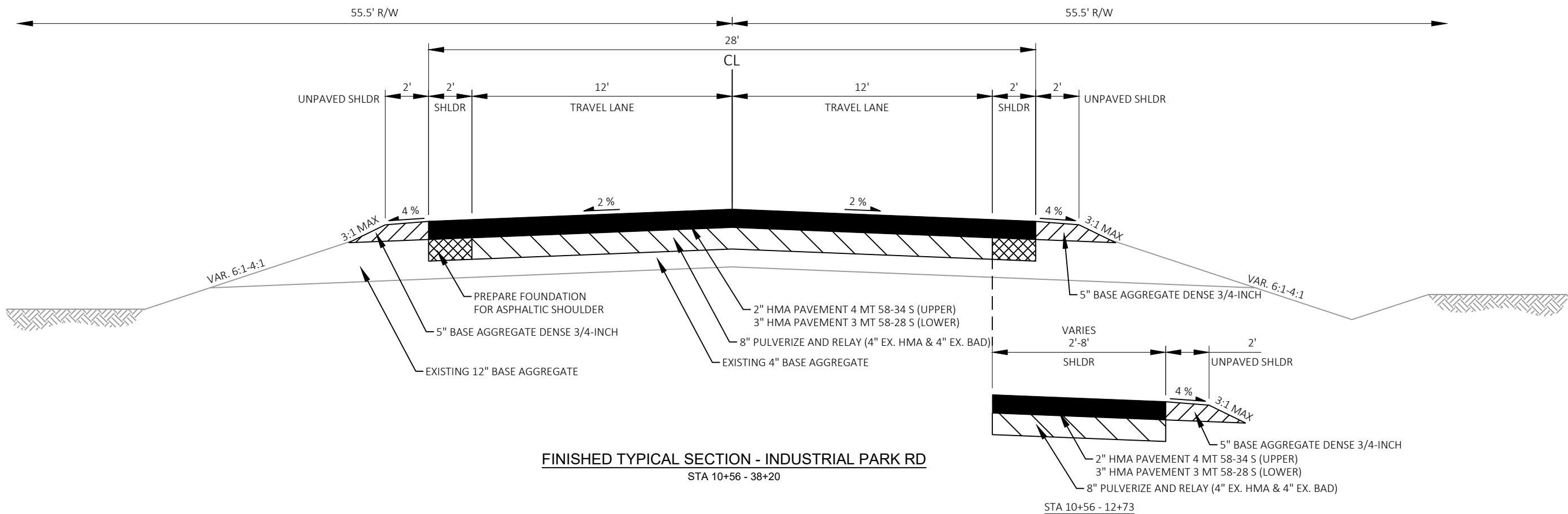
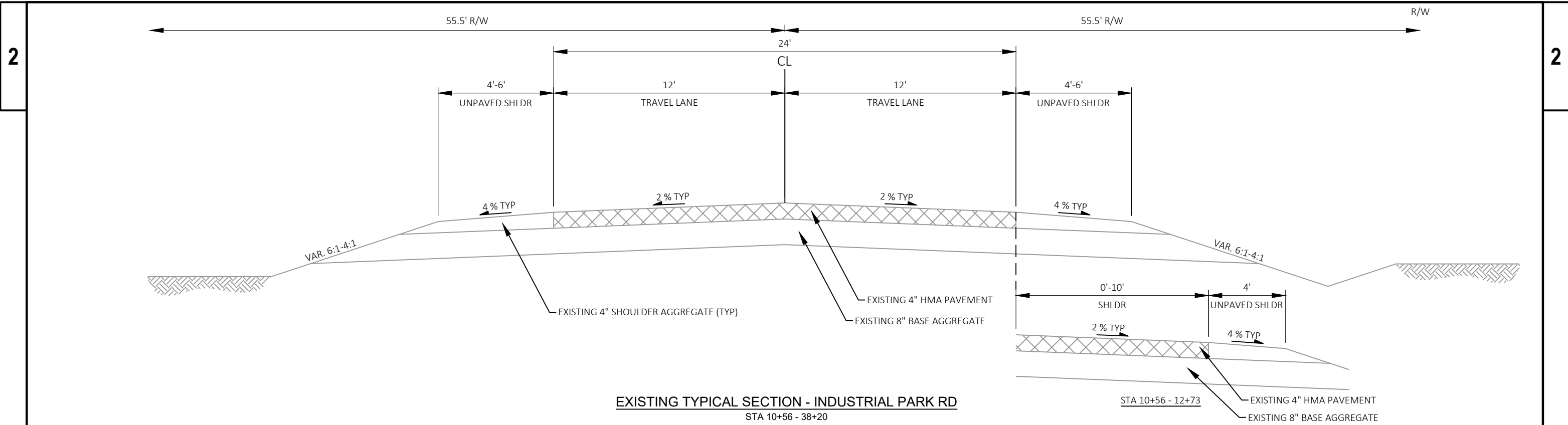
HWY: INDUSTRIAL PARK ROAD

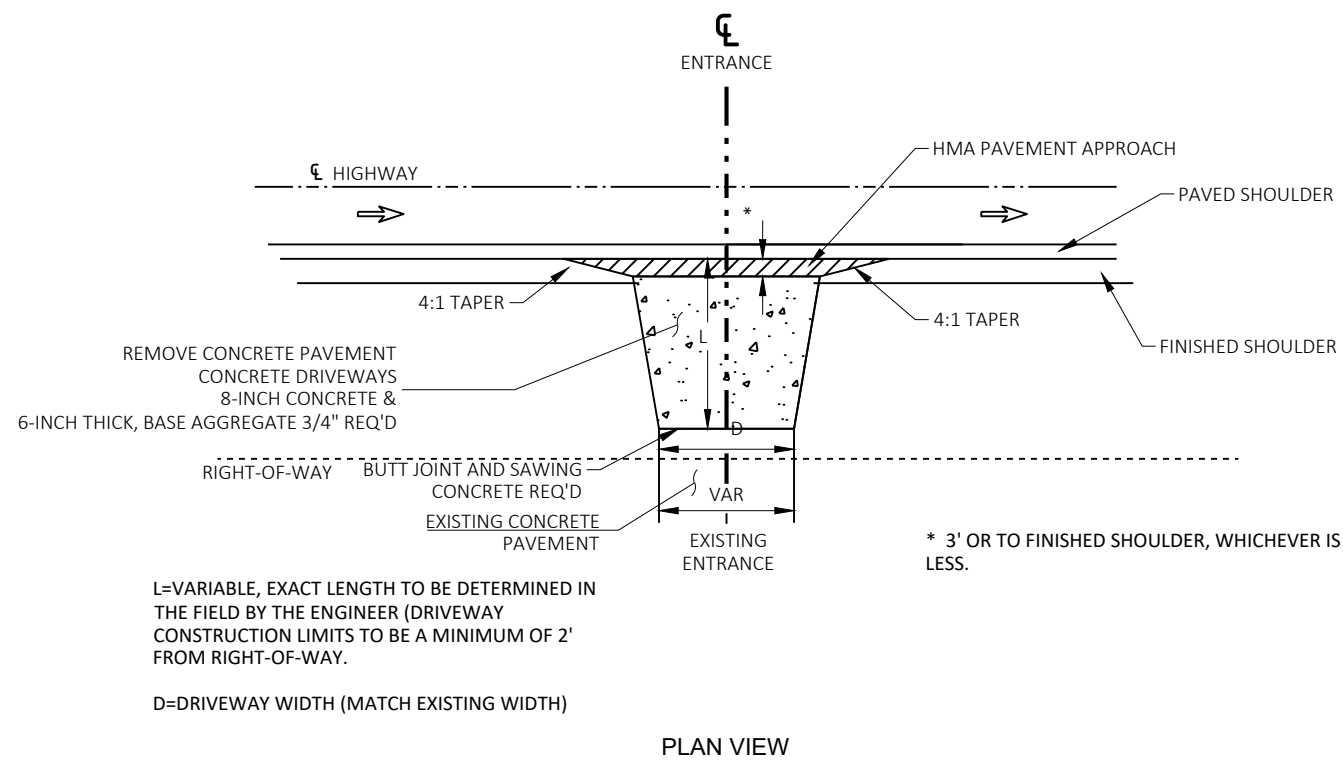
COUNTY: ASHLAND

PROJECT OVERVIEW

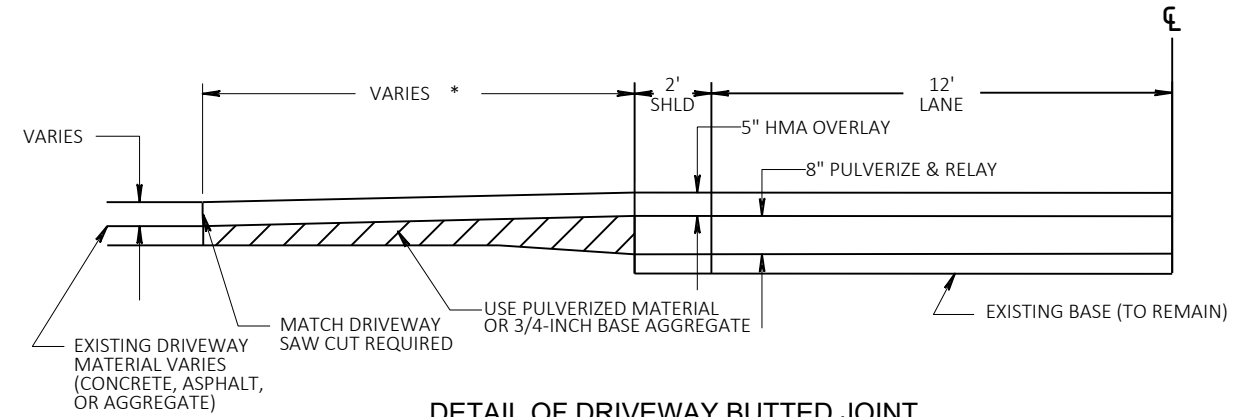
SHEET

E



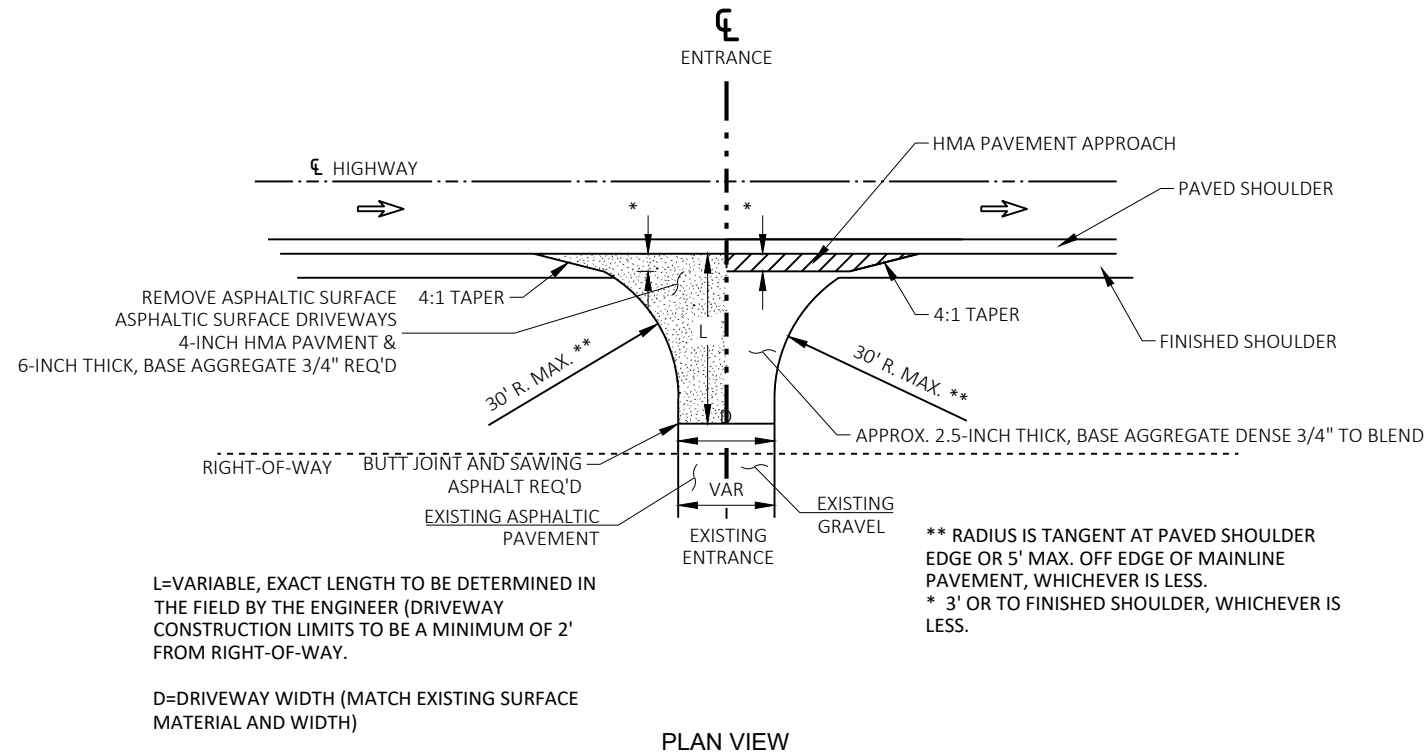


RURAL CONCRETE DRIVEWAY DETAIL (CE)
(FOR RESURFACING PROJECTS)

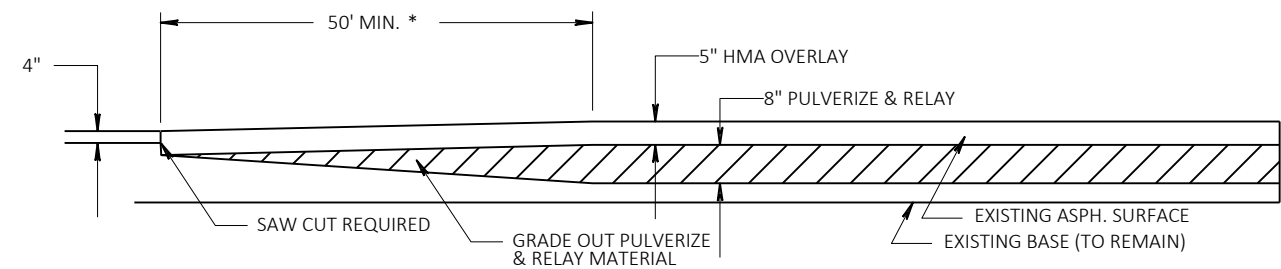


DETAIL OF DRIVEWAY BUTTED JOINT

* EXACT DIMENSIONS TO BE DETERMINED BY THE ENGINEER IN THE FIELD.



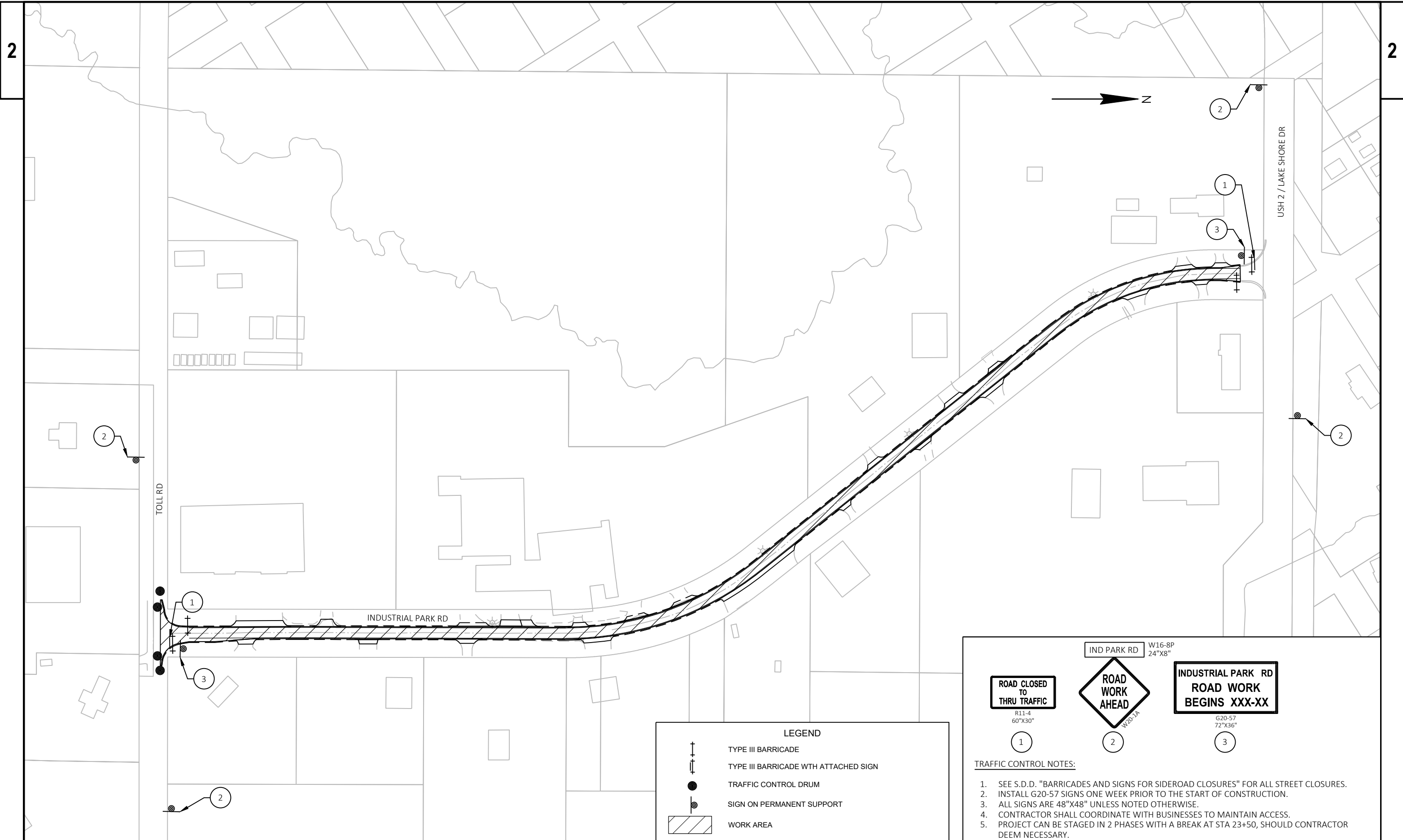
RURAL DRIVEWAY DETAIL (PE, CE & FE)
(FOR RESURFACING PROJECTS)



DETAIL OF MAINLINE BUTTED JOINT

STA 10+56 & STA 38+20

* EXACT DIMENSIONS TO BE DETERMINED BY THE ENGINEER IN THE FIELD.

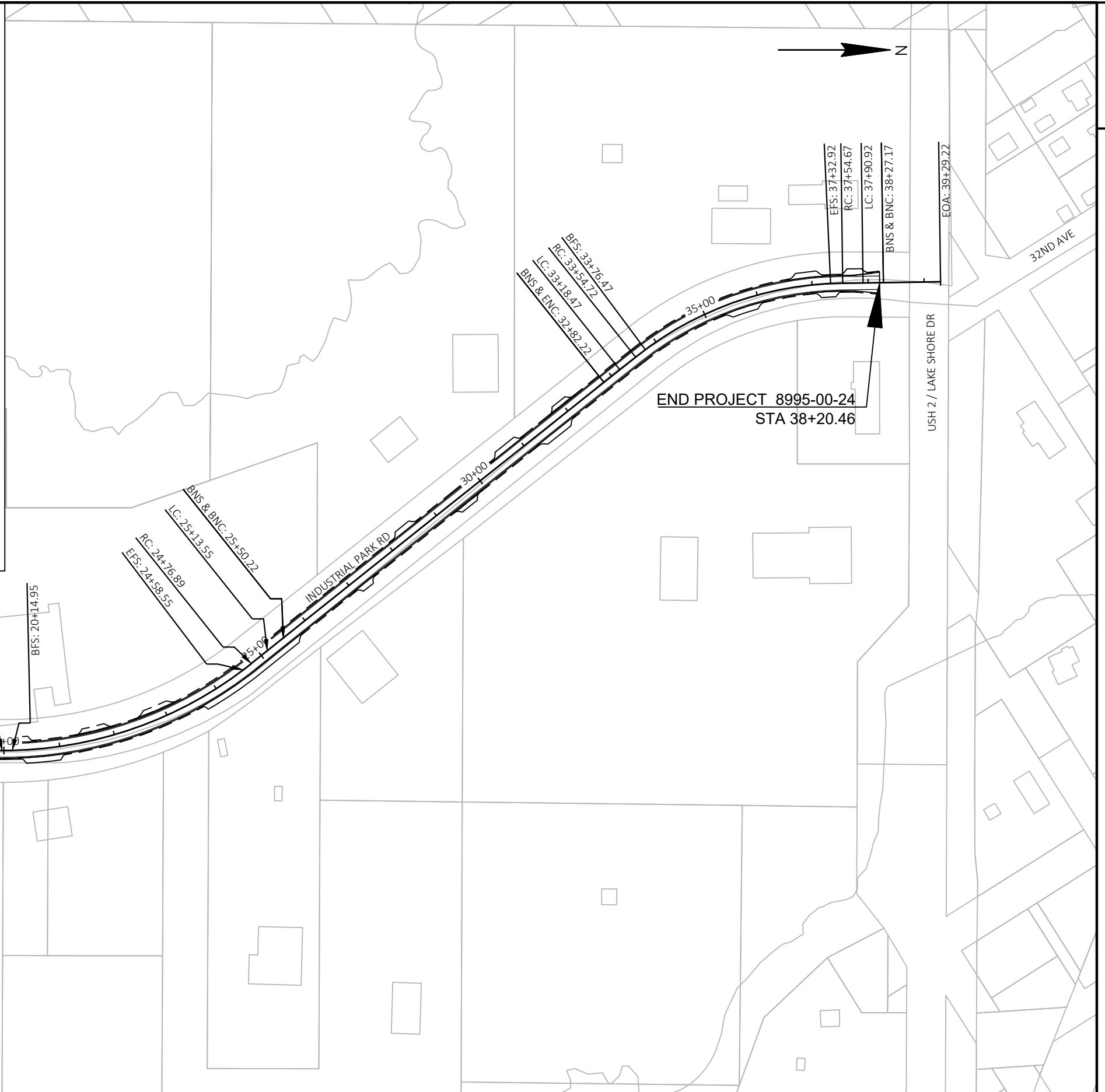


LEGEND	
	TYPE III BARRICADE
	TYPE III BARRICADE WITH ATTACHED SIGN
	TRAFFIC CONTROL DRUM
	SIGN ON PERMANENT SUPPORT
	WORK AREA

IND PAK RD W16-8P 24"x8"	IND PAK RD W20-1A 24"x8"	IND PAK RD W20-1A 24"x8"
R11-4 60"x30"	G20-57 72"x36"	G20-57 72"x36"
1	2	3

- TRAFFIC CONTROL NOTES:**
- SEE S.D.D. "BARRICADES AND SIGNS FOR SIDEROAD CLOSURES" FOR ALL STREET CLOSURES.
 - INSTALL G20-57 SIGNS ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
 - ALL SIGNS ARE 48"x48" UNLESS NOTED OTHERWISE.
 - CONTRACTOR SHALL COORDINATE WITH BUSINESSES TO MAINTAIN ACCESS.
 - PROJECT CAN BE STAGED IN 2 PHASES WITH A BREAK AT STA 23+50, SHOULD CONTRACTOR DEEM NECESSARY.

STATION	DESCRIPTION	LT. SHOULDER	LT. LANE	RT. SHOULDER	RT. LANE
10+00.00	Begin Alignment	-4.00%	-2.00%	-4.00%	-2.00%
19+23.28	End Normal Shoulder	-4.00%	-2.00%	-4.00%	-2.00%
19+23.28	End Normal Crown	-4.00%	-2.00%	-4.00%	-2.00%
19+59.95	Level Crown	-4.00%	-2.00%	0.00%	0.00%
19+96.62	Reverse Crown	-4.00%	-2.00%	2.00%	2.00%
20+14.95	Begin Full Super	-4.00%	-3.00%	3.00%	3.00%
24+58.55	End Full Super	-4.00%	-3.00%	3.00%	3.00%
24+76.89	Reverse Crown	-4.00%	-2.00%	2.00%	2.00%
25+13.55	Level Crown	-4.00%	-2.00%	0.00%	0.00%
25+50.22	Begin Normal Crown	-4.00%	-2.00%	-4.00%	-2.00%
25+50.22	Begin Normal Shoulder	-4.00%	-2.00%	-4.00%	-2.00%
32+82.22	End Normal Shoulder	-4.00%	-2.00%	-4.00%	-2.00%
32+82.22	End Normal Crown	-4.00%	-2.00%	-4.00%	-2.00%
33+18.47	Level Crown	0.00%	0.00%	-4.00%	-2.00%
33+54.72	Reverse Crown	2.00%	2.00%	-4.00%	-2.00%
33+76.47	Begin Full Super	3.20%	3.20%	-4.00%	-3.20%
37+32.92	End Full Super	3.20%	3.20%	-4.00%	-3.20%
37+54.67	Reverse Crown	2.00%	2.00%	-4.00%	-2.00%
37+90.92	Level Crown	0.00%	0.00%	-4.00%	-2.00%
38+27.17	Begin Normal Crown	-4.00%	-2.00%	-4.00%	-2.00%
38+27.17	Begin Normal Shoulder	-4.00%	-2.00%	-4.00%	-2.00%
39+29.22	End Alignment	-4.00%	-2.00%	-4.00%	-2.00%



Estimate Of Quantities By Plan Sets

8995-00-24

Line	Item	Item Description	Unit	Total	Qty
0002	204.0100	Removing Concrete Pavement	SY	127.000	127.000
0004	204.0110	Removing Asphaltic Surface	SY	1,545.000	1,545.000
0010	211.0400	Prepare Foundation for Asphaltic Shoulders	STA	55.000	55.000
0014	213.0100	Finishing Roadway (project) 02. 8995-00-24	EACH	1.000	1.000
0016	305.0110	Base Aggregate Dense 3/4-Inch	TON	831.000	831.000
0018	325.0100	Pulverize and Relay	SY	8,840.000	8,840.000
0020	455.0605	Tack Coat	GAL	619.000	619.000
0022	460.2000	Incentive Density HMA Pavement	DOL	1,590.000	1,590.000
0026	460.6223	HMA Pavement 3 MT 58-28 S	TON	1,490.000	1,490.000
0028	460.6244	HMA Pavement 4 MT 58-34 S	TON	990.000	990.000
0030	465.0120	Asphaltic Surface Driveways and Field Entrances	TON	364.000	364.000
0032	602.0820	Concrete Driveway 8-Inch	SY	127.000	127.000
0034	611.0530	Manhole Covers Type J	EACH	2.000	2.000
0038	618.0100	Maintenance and Repair of Haul Roads (project) 02. 8995-00-24	EACH	1.000	1.000
0040	619.1000	Mobilization	EACH	0.650	0.650
0042	624.0100	Water	MGAL	13.000	13.000
0044	628.1905	Mobilizations Erosion Control	EACH	2.000	2.000
0046	628.1910	Mobilizations Emergency Erosion Control	EACH	2.000	2.000
0048	628.7005	Inlet Protection Type A	EACH	6.000	6.000
0050	628.7015	Inlet Protection Type C	EACH	1.000	1.000
0052	628.7555	Culvert Pipe Checks	EACH	14.000	14.000
0054	642.5001	Field Office Type B	EACH	0.500	0.500
0056	643.0300	Traffic Control Drums	DAY	350.000	350.000
0058	643.0420	Traffic Control Barricades Type III	DAY	140.000	140.000
0060	643.0705	Traffic Control Warning Lights Type A	DAY	280.000	280.000
0062	643.0900	Traffic Control Signs	DAY	280.000	280.000
0064	643.1000	Traffic Control Signs Fixed Message	SF	36.000	36.000
0066	643.5000	Traffic Control	EACH	0.500	0.500
0074	650.8000	Construction Staking Resurfacing Reference	LF	2,765.000	2,765.000
0078	650.9911	Construction Staking Supplemental Control (project) 02. 8995-00-24	EACH	1.000	1.000
0080	690.0150	Sawing Asphalt	LF	924.000	924.000
0082	690.0250	Sawing Concrete	LF	85.000	85.000
0084	740.0440	Incentive IRI Ride	DOL	2,100.000	2,100.000
0086	ASP.1T0A	On-the-Job Training Apprentice at \$5.00/HR	HRS	600.000	600.000
0088	ASP.1T0G	On-the-Job Training Graduate at \$5.00/HR	HRS	600.000	600.000
0094	SPV.0060	Special 03. Reconstructing Sanitary Manholes	EACH	4.000	4.000
0096	SPV.0060	Special 04. Adjusting Sanitary Manhole Covers	EACH	6.000	6.000
0098	SPV.0060	Special 05. Adjusting Water Manhole Covers	EACH	1.000	1.000

PAVEMENT REMOVAL

CATEGORY	STATION	TO	STATION	LOCATION	204.0100 REMOVING CONCRETE PAVEMENT SY	204.0110 REMOVING ASPHALTIC SURFACE SY	325.0100 PULVERIZE AND RELAY SY
0010	10+56	-	38+20	INDUSTRIAL PARK RD	-	-	8,840
0010	12+12	-	13+60	LT	-	188	-
0010	12+25	-	12+86	RT	-	52	-
0010	14+10	-	14+75	LT	-	73	-
0010	15+07	-	15+69	RT	48	48	-
0010	17+34	-	18+03	RT	-	51	-
0010	17+36	-	18+30	LT	-	61	-
0010	18+36	-	18+96	LT	79	79	-
0010	18+96	-	19+39	LT	-	48	-
0010	20+02	-	20+60	LT	-	40	-
0010	20+24	-	21+13	RT	-	79	-
0010	21+28	-	22+06	LT	-	44	-
0010	22+42	-	23+31	LT	-	50	-
0010	23+57	-	25+86	RT	-	234	-
0010	26+99	-	27+52	RT	-	35	-
0010	28+07	-	28+73	LT	-	47	-
0010	29+24	-	29+91	RT	-	37	-
0010	30+36	-	31+11	LT	-	60	-
0010	31+17	-	32+07	RT	-	73	-
0010	31+66	-	32+29	LT	-	33	-
0010	35+26	-	36+14	RT	-	68	-
0010	36+55	-	37+27	LT	-	56	-
0010	36+96	-	37+58	RT	-	42	-
0010	37+44	-	38+08	LT	-	47	-
TOTAL 0010					127	1545	8840

BASE AGGREGATE

CATEGORY	STATION	TO	STATION	LOCATION	305.0110 BASE AGGREGATE DENSE 3/4-INCH TON	624.0100 WATER MGAL	REMARKS
0010	10+56	-	38+20	LT/RT	310	5.0	SHOULDERS
TOTAL 0010					310	5.0	

* QUANTITIES FOUND ELSEWHERE ON PLANS

HMA PAVING

CATEGORY	STATION	TO	STATION	LOCATION	455.0605 TACK COAT GAL	460.6223 HMA PAVEMENT 3 MT 58-28 S TON	460.6244 HMA PAVEMENT 4 MT 58-34 S TON	REMARKS
0010	10+56	-	38+20	INDUSTRIAL PARK RD	619			
0010	10+56	-	38+20	INDUSTRIAL PARK RD		1490		LOWER
0010	10+56	-	38+20	INDUSTRIAL PARK RD			990	UPPER
TOTAL 0010					619	1,490	990	

DRIVEWAYS

CATEGORY	STATION	TO	STATION	LOCATION	305.0110 BASE AGGREGATE DENSE 3/4-INCH TON	465.0120 ASPHALTIC SURFACE DRIVEWAYS AND FIELD ENTRANCES TON	602.0820 CONCRETE DRIVEWAY 8- INCH SY	624.0100 WATER MGAL	REMARKS
0010	12+12	-	13+60	LT	58	53			721 SOUTH DWY
0010	12+25	-	12+86	RT	16	15			800 DWY
0010	14+10	-	14+75	LT	23	21			721 NORTH DWY
0010	15+07	-	15+69	RT	19	4	48		710 DWY
0010	17+34	-	18+03	RT	16	15			610 DWY
0010	17+36	-	18+30	LT	25	6			601 SOUTH GRVL DWY
0010	18+36	-	18+96	LT	29	4	79		601 CONC DWY
0010	18+96	-	19+39	LT	18	3			601 GRVL DWY
0010	20+02	-	20+60	LT	13	12			601 ASPH DWY
0010	20+24	-	21+13	RT	25	23			600 DWY
0010	21+28	-	22+06	LT	19	5			601 GRVL DWY
0010	22+42	-	23+31	LT	21	5			601 NORTH GRVL DWY
0010	23+57	-	25+86	RT	73	66			510 DWY
0010	26+99	-	27+52	RT	11	10			410 DWY
0010	28+07	-	28+73	LT	15	14			405 SOUTH DWY
0010	29+24	-	29+91	RT	16	4			310 SOUTH DWY
0010	30+36	-	31+11	LT	19	17			405 NORTH DWY
0010	31+17	-	32+07	RT	23	21			310 MIDDLE DWY
0010	31+66	-	32+29	LT	14	4			3122 SOUTH DWY
0010	35+26	-	36+14	RT	22	20			310 NORTH DWY
0010	36+55	-	37+27	LT	18	16			3122 MIDDLE DWY
0010	36+96	-	37+58	RT	13	12			3200 DWY
0010	37+44	-	38+08	LT	15	14			3122 NORTH DWY
0010	10+56	-	38+20	LT/RT				8.0	ALL DRIVEWAYS
TOTAL 0010					521	364	127	8.0	

* QUANTITIES FOUND ELSEWHERE ON PLANS

EROSION CONTROL

CATEGORY	LOCATION	628.1905 MOBILIZATIONS EROSION CONTROL EACH	628.1910 MOBILIZATIONS EMERGENCY EROSION CONTROL EACH	628.7005 INLET PROTECTION TYPE A EACH	628.7015 INLET PROTECTION TYPE C EACH	628.7555 CULVERT PIPE CHECKS EACH
0010	PROJECT			6	1	14
0010	PROJECT	2	2	6	1	14
	TOTAL 0010	2	2	6	1	14

TRAFFIC CONTROL

CATEGORY	LOCATION	643.0300 TRAFFIC CONTROL DRUMS QTY	643.0300 TRAFFIC CONTROL DRUMS DAY	643.0420 TRAFFIC CONTROL BARRICADES TYPE III QTY	643.0420 TRAFFIC CONTROL BARRICADES TYPE III DAY	643.0705 TRAFFIC CONTROL WARNING LIGHTS TYPE A QTY	643.0705 TRAFFIC CONTROL WARNING LIGHTS TYPE A DAY	643.0900 TRAFFIC CONTROL SIGNS QTY	643.0900 TRAFFIC CONTROL SIGNS DAY	643.1000 TRAFFIC CONTROL SIGNS FIXED MESSAGE SF	643.5000 TRAFFIC CONTROL EACH
0010	CARY ST 7-DAY ADVANCED WARNING PROJECT	10	350	4	140	8	280	8	280	36	0.5
	TOTAL 0010		350		140		280		280	36	0.5

CONSTRUCTION STAKING

CATEGORY	STATION	TO	STATION	LOCATION	650.8000 CONSTRUCTION STAKING RESURFACING REFERENCE LF	650.9911.02 CONSTRUCTION STAKING SUPPLEMENTAL CONTROL (PROJECT) (02. 8995-00-24) EACH
0010	10+56	-	38+20	INDUSTRIAL PARK RD PROJECT	2,765	1
0010				TOTAL 0010	2,765	1

SAWING

CATEGORY	STATION	TO	STATION	LOCATION	690.0150 SAWING ASPHALT LF	690.0250 SAWING CONCRETE LF
0010	10+56			TOLL RD	157	
0010	12+32	-	13+41	DRIVEWAY LT	109	
0010	12+37	-	12+71	DRIVEWAY RT	35	
0010	15+19	-	15+59	DRIVEWAY RT		40
0010	17+52	-	17+85	DRIVEWAY RT	34	
0010	18+47	-	18+92	DRIVEWAY LT		45
0010	20+15	-	20+45	DRIVEWAY LT	29	
0010	20+38	-	20+99	DRIVEWAY RT	63	
0010	23+69	-	25+68	DRIVEWAY RT	203	
0010	27+14	-	27+37	DRIVEWAY RT	23	
0010	28+24	-	28+56	DRIVEWAY LT	32	
0010	30+52	-	30+97	DRIVEWAY LT	45	
0010	31+35	-	31+88	DRIVEWAY RT	53	
0010	35+45	-	35+95	DRIVEWAY RT	48	
0010	36+73	-	37+10	DRIVEWAY LT	39	
0010	37+14	-	37+42	DRIVEWAY RT	27	
0010	37+62	-	37+89	DRIVEWAY LT	27	
				TOTAL 0010	924	85

3

FIELD OFFICE

CATEGORY	LOCATION	642.5001 FIELD OFFICE TYPE B EACH
0010	PROJECT	0.5
	TOTAL 0010	0.5

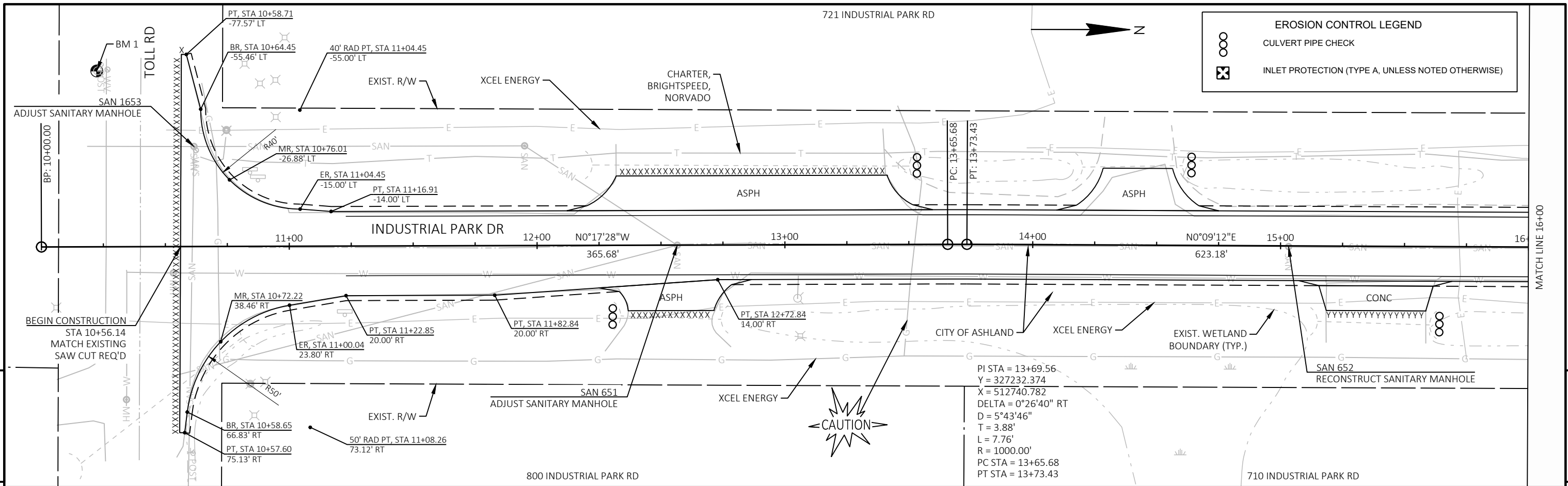
MOBILIZATION

CATEGORY	LOCATION	619.1000 MOBILIZATION EACH
0010	PROJECT	0.65
	TOTAL 0010	0.65

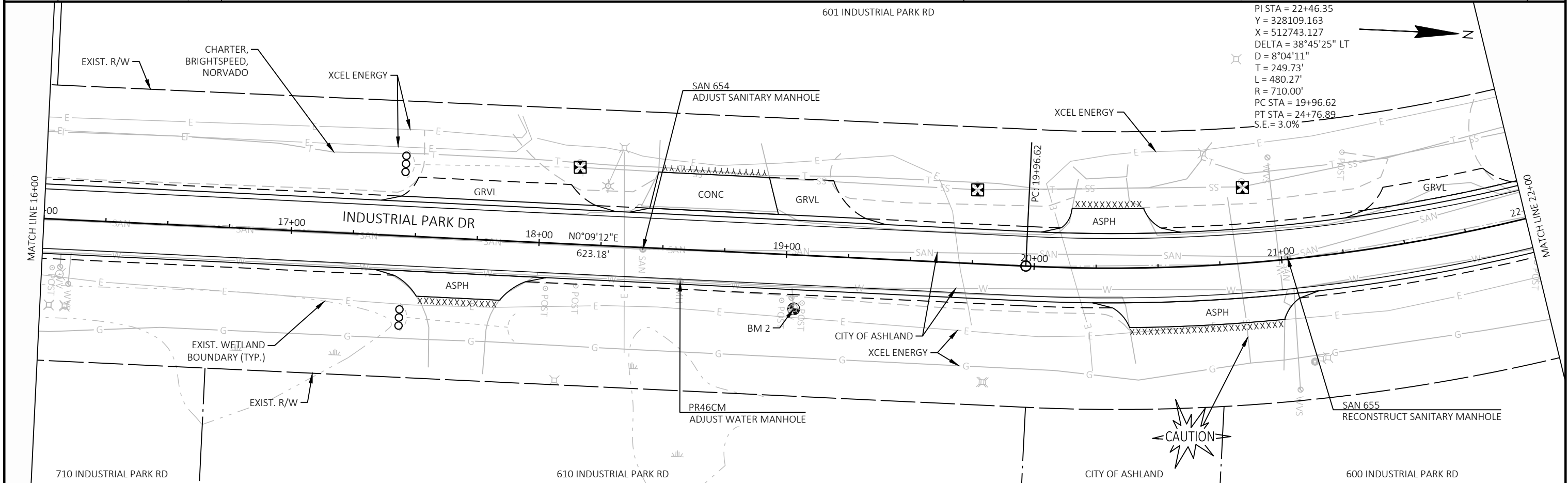
CATEGORY 20

CATEGORY	STATION	OFFSET	LOCATION	611.0530 MANHOLE COVERS TYPE J EACH	618.0100.02 MAINTENANCE AND REPAIR OF HAUL ROADS (PROJECT) (02. 8995-00-24) EACH	SPV.0060.04 SPECIAL (04. ADJUSTING SANITARY MANHOLE COVERS) EACH	SPV.0060.03 SPECIAL (03. RECONSTRUCTING SANITARY MANHOLES) EACH	SPV.0060.05 SPECIAL (05. ADJUSTING WATER MANHOLE COVERS) EACH	REMARKS
0020	10+62	LT				1			SAN 1653
0020	12+57	RT				1			SAN 651
0020	15+03	RT		1			1		SAN 652 (REPLACE ADJ. RINGS)
0020	18+42	RT				1			SAN 654
0020	18+58	RT						1	PR46CM
0020	21+02	LT					1		SAN 655 (REPLACE ADJ. RINGS)
0020	23+55	RT				1			SAN 656
0020	27+34	RT				1			SAN 657
0020	31+37	RT				1			SAN 658
0020	34+34	RT		1			1		SAN 659 (REPLACE ADJ. RINGS)
0020	36+35	LT					1		SAN 660 (REPLACE ADJ. RINGS)
			PROJECT			1			
			TOTAL 0020	2	1	6	4	1	

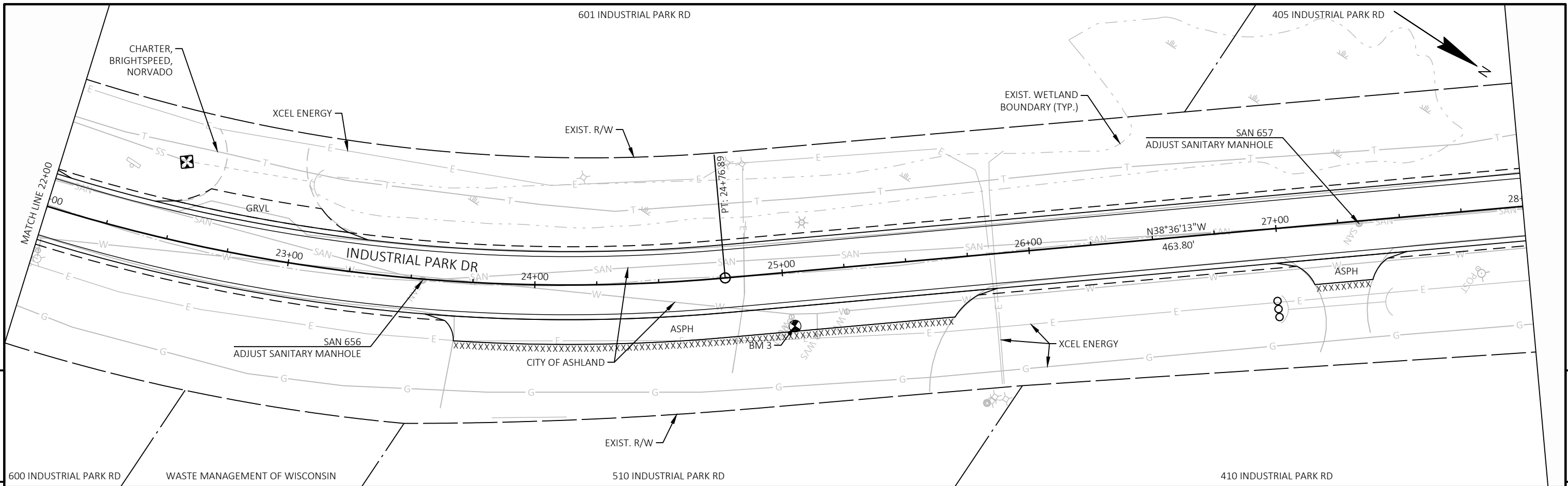
3



EROSION CONTROL LEGEND	
	CULVERT PIPE CHECK
	INLET PROTECTION (TYPE A, UNLESS NOTED OTHERWISE)

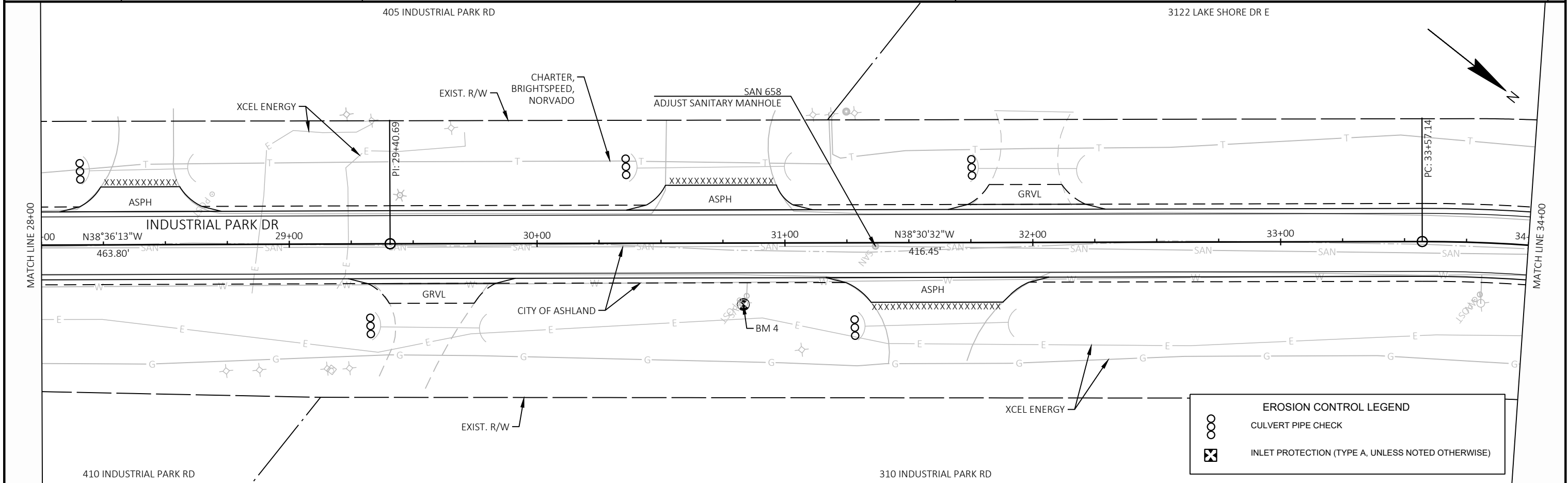


PROJECT NO: 8995-00-24	HWY: INDUSTRIAL PARK ROAD	COUNTY: ASHLAND	PLAN	SHEET	E
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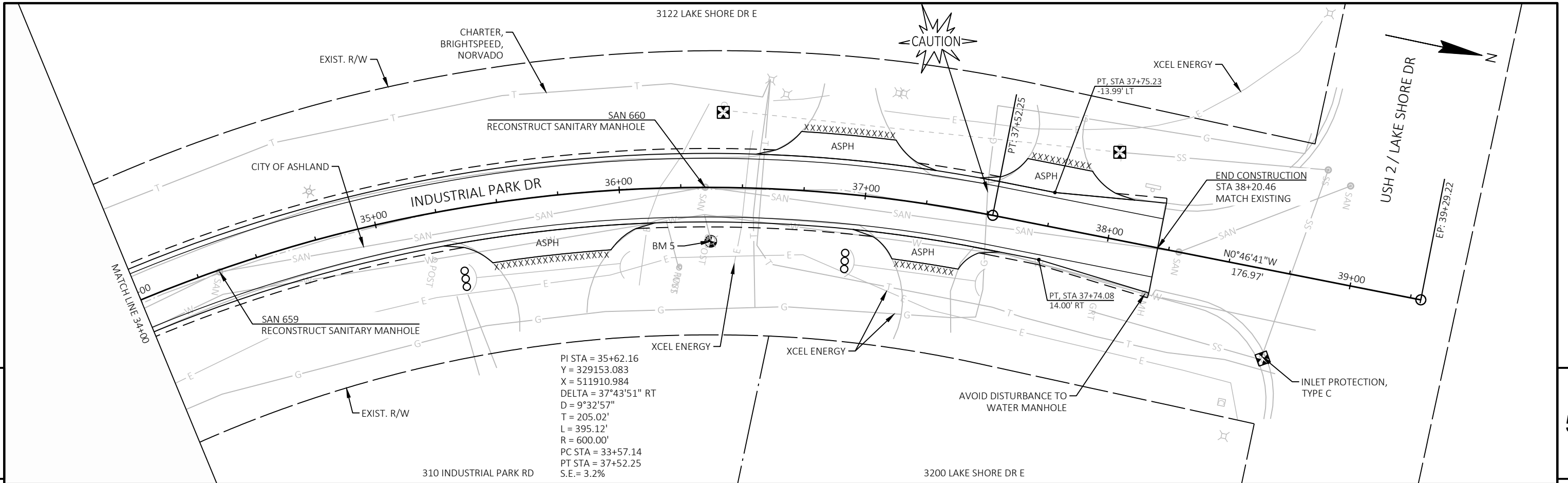


EROSION CONTROL LEGEND

-  CULVERT PIPE CHECK
-  INLET PROTECTION (TYPE A, UNLESS NOTED OTHERWISE)

PROJECT NO: 8995-00-24 HWY: INDUSTRIAL PARK ROAD COUNTY: ASHLAND PLAN SHEET E

FILE NAME: 050201-PN.DWG PLOT DATE: 2/1/2024 9:01 AM PLOT BY: JORDAN DISTERHAFT PLOT NAME: PLOT SCALE: 1 IN:40 FT WISDOT/CADD SHEET 44



5

5

EROSION CONTROL LEGEND	
	CULVERT PIPE CHECK
	INLET PROTECTION (TYPE A, UNLESS NOTED OTHERWISE)

THIS PANEL WAS INTENTIONALLY LEFT BLANK

Standard Detail Drawing List

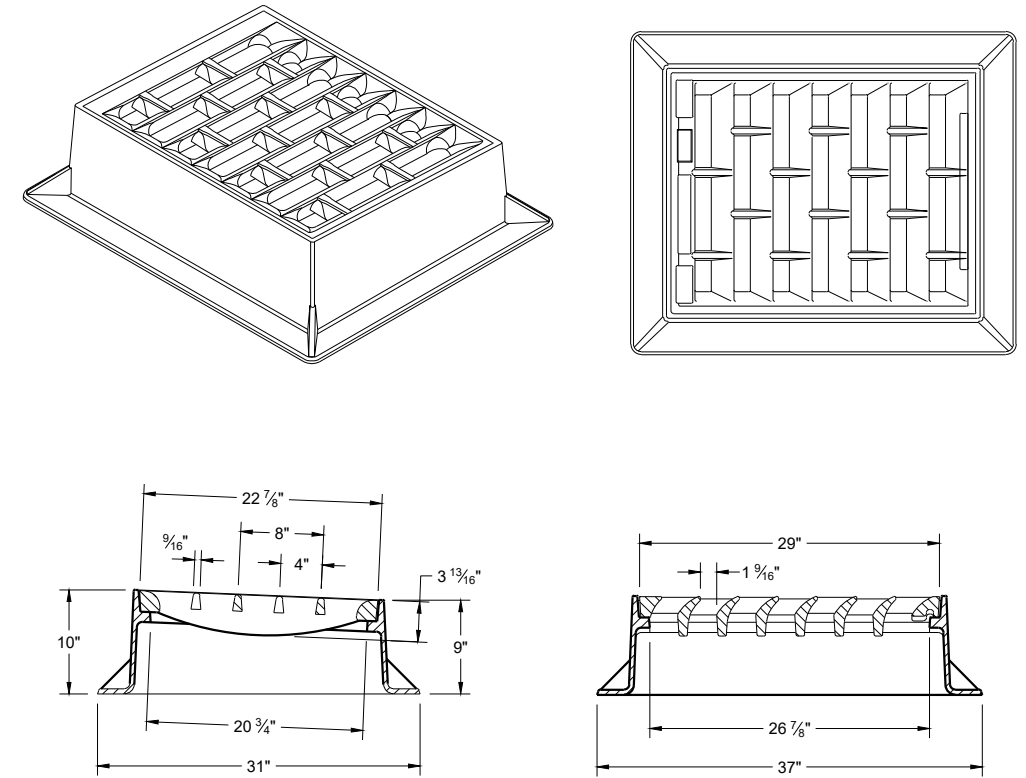
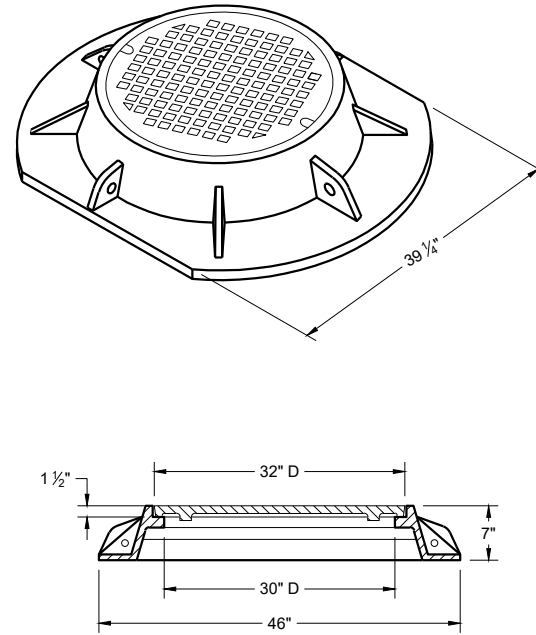
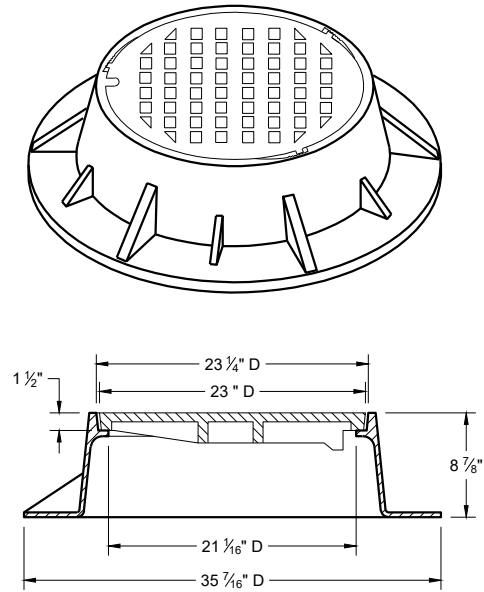
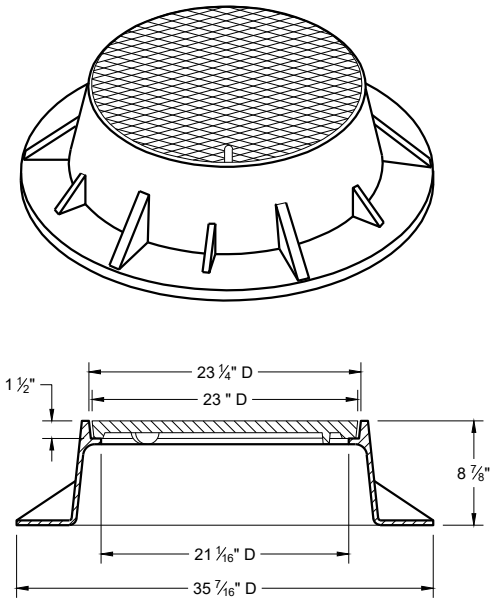
08A05-20D	INLET COVER TYPE BW, MANHOLE COVERS, TYPE K, J, J-S, L & M
08E10-02	INLET PROTECTION TYPE A, B, C AND D
08E15-01	CULVERT PIPE CHECK
13C19-03	HMA LONGITUDINAL JOINTS
15C02-09A	BARRICADES AND SIGNS FOR MAINLINE CLOSURES
15C02-09B	BARRICADES AND SIGNS FOR VARIOUS CLOSURES
15C03-05	BARRICADES AND SIGNS FOR SIDEROAD CLOSURES
15C05-05	TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40 M. P. H. OR LESS
15C12-09A	TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

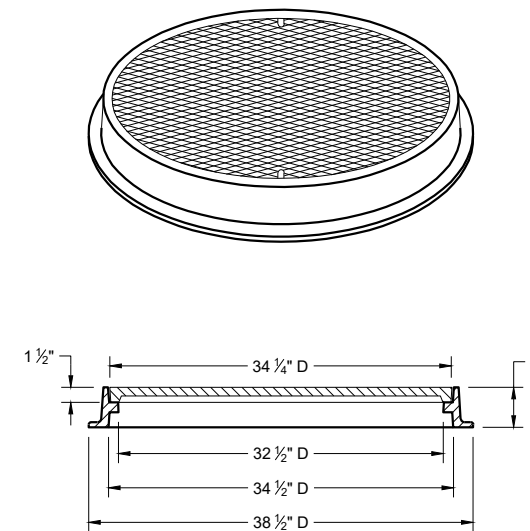
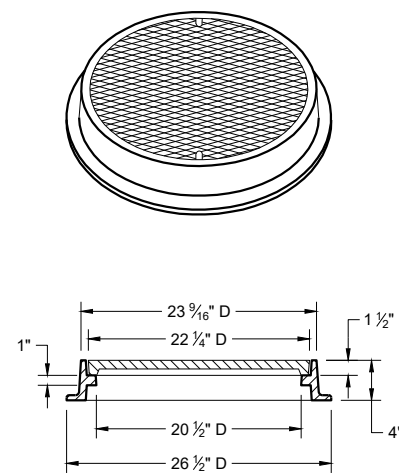
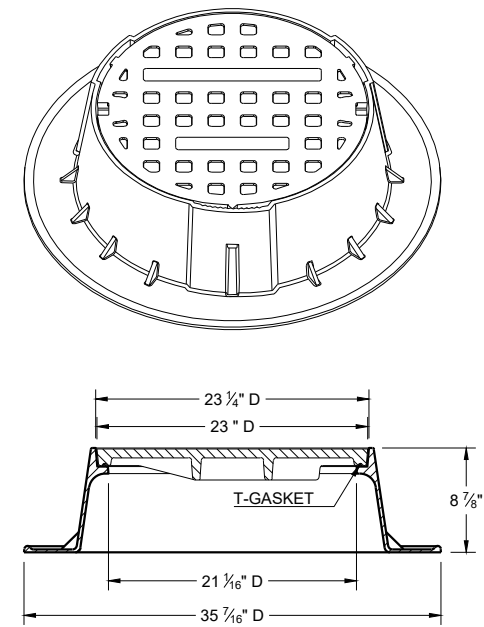
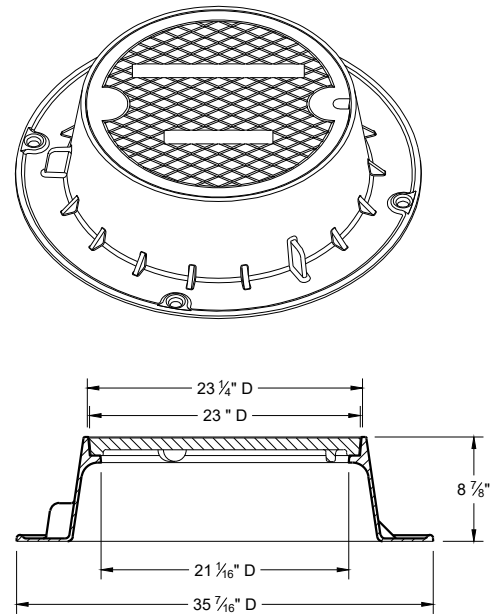
DETAIL DRAWINGS FOR PROPOSED ALTERNATE DESIGNS FOR MANHOLE COVERS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL PROVIDING THAT SUCH ALTERNATE DESIGNS MAKE PROVISION FOR EQUIVALENT CAPACITY AND STRENGTH.

ROUND FRAMES AND COVERS SHALL HAVE CONTINUOUSLY MACHINED BEARING SURFACES TO PREVENT ROCKING AND RATTLING.



TYPE "K"

INLET COVER TYPE "BW"



TYPE "J"

NOTE: EITHER CASTING IS ACCEPTABLE

TYPE "J" SPECIAL

TYPE "B" NON-ROCKING SELF-SEAL LID (NOTED AS TYPE J-S ON THE DRAINAGE TABLE)

NOTE: EITHER CASTING IS ACCEPTABLE

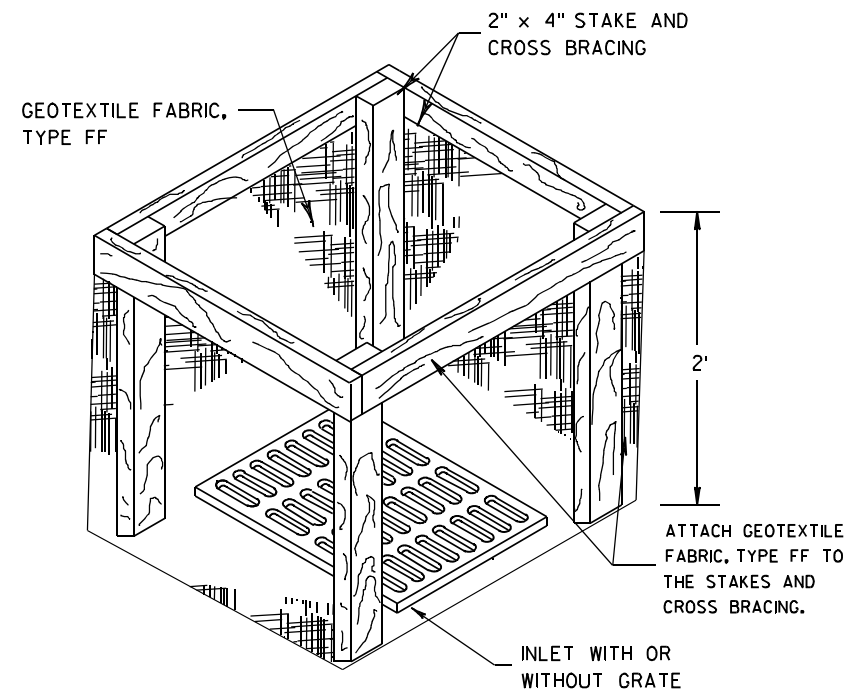
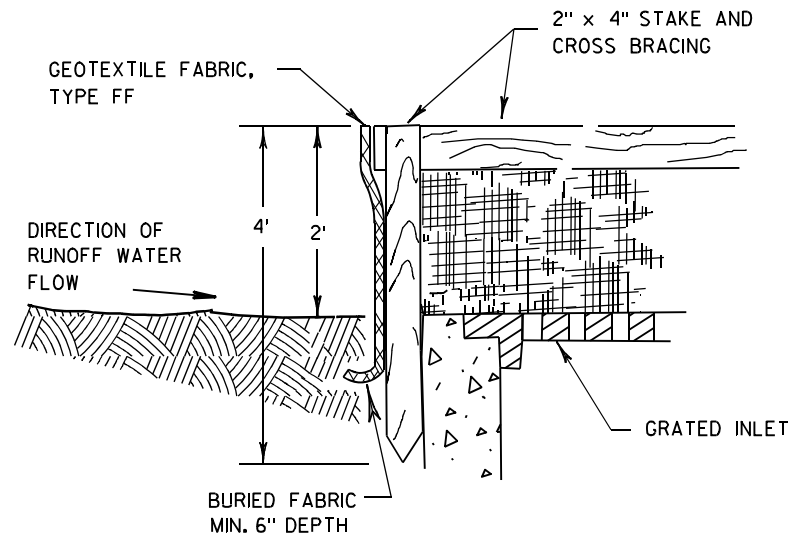
TYPE "L"

TYPE "M"

**INLET COVERS TYPES BW
MANHOLE COVERS TYPES K,
J, J-S, L, AND M**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
July 2023 /S/ Rodney Taylor
DATE ROADWAY STANDARDS DEVELOPMENT
UNIT SUPERVISOR



INLET PROTECTION, TYPE A

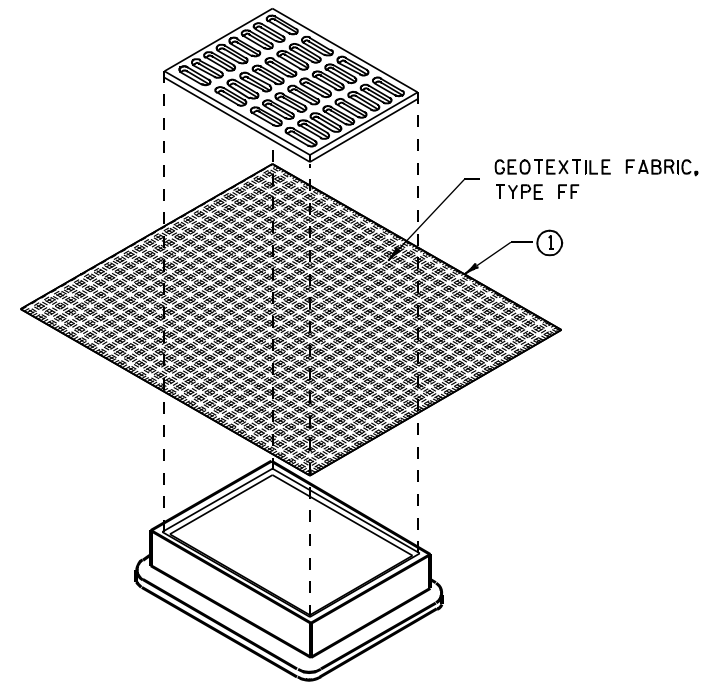
GENERAL NOTES

INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.

MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.

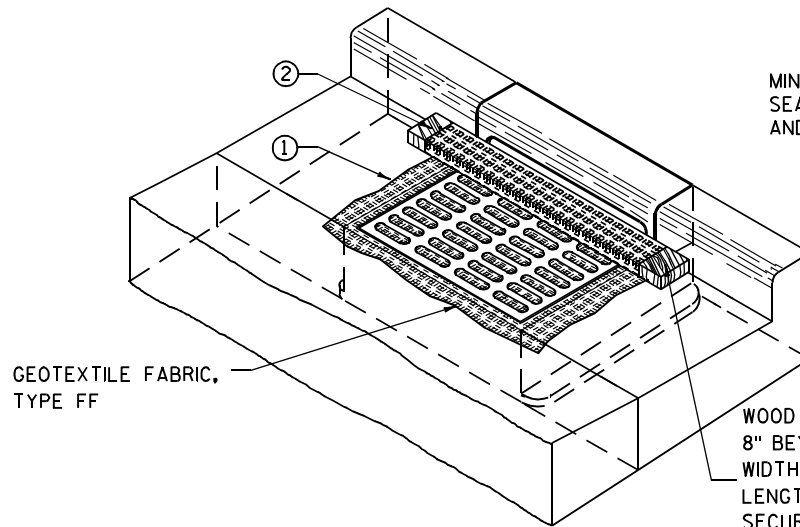
WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.

- ① FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
- ② FOR INLET PROTECTION, TYPE C (WITH CURB BOX), AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
- ③ FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2X4.



**INLET PROTECTION, TYPE B
(WITHOUT CURB BOX)**

(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)



INLET PROTECTION, TYPE C (WITH CURB BOX)

INSTALLATION NOTES

TYPE B & C

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

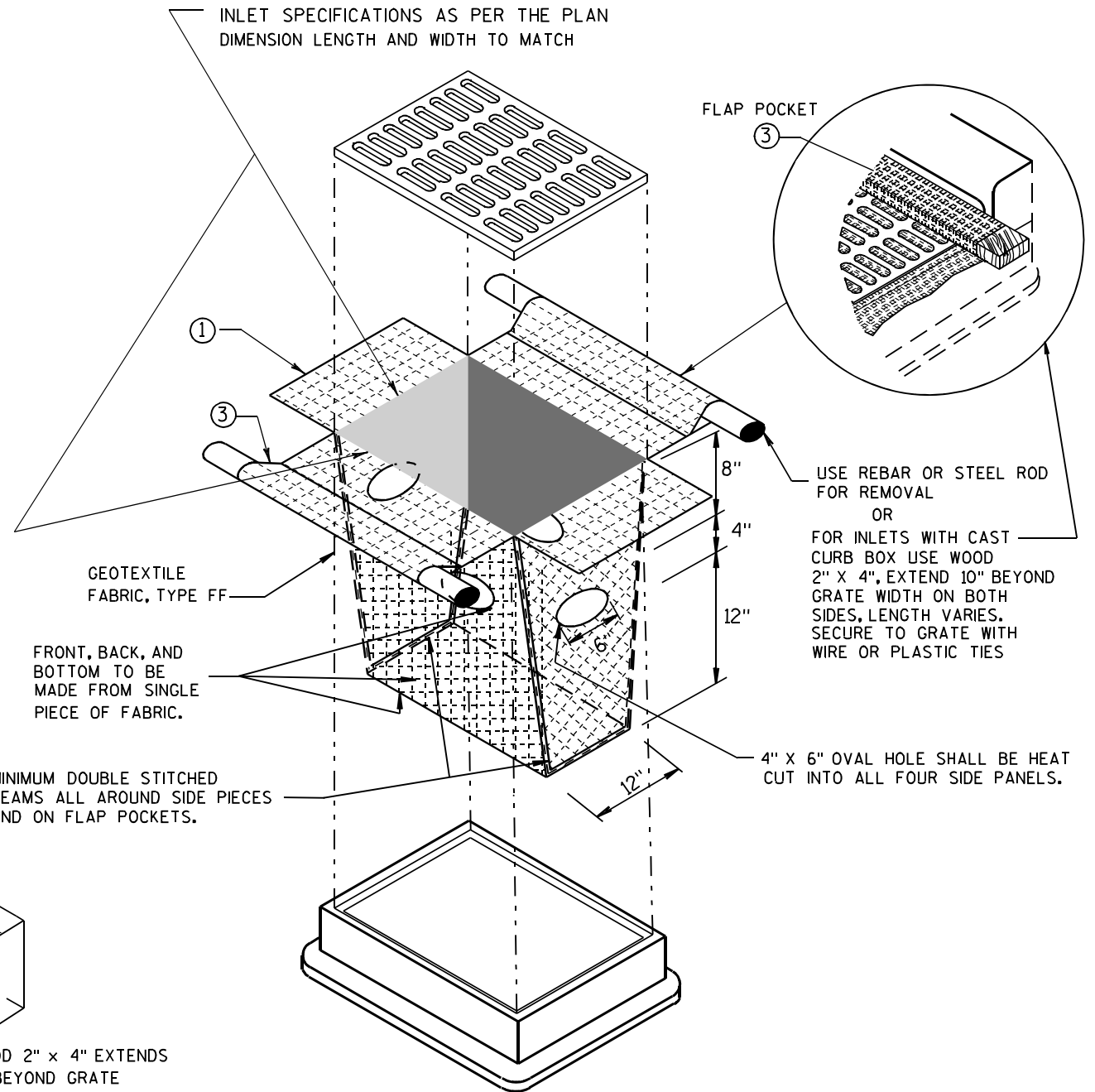
THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

TYPE D

DO NOT INSTALL INLET PROTECTION TYPE D IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

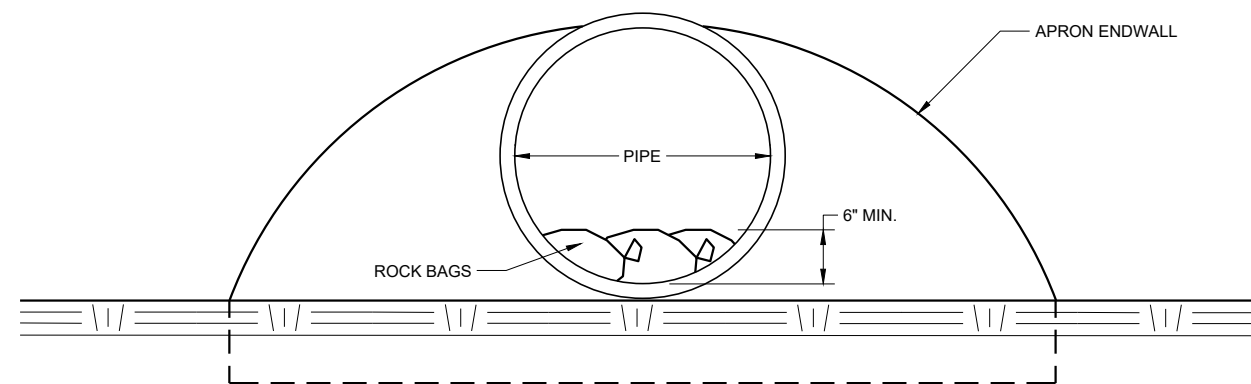
THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.



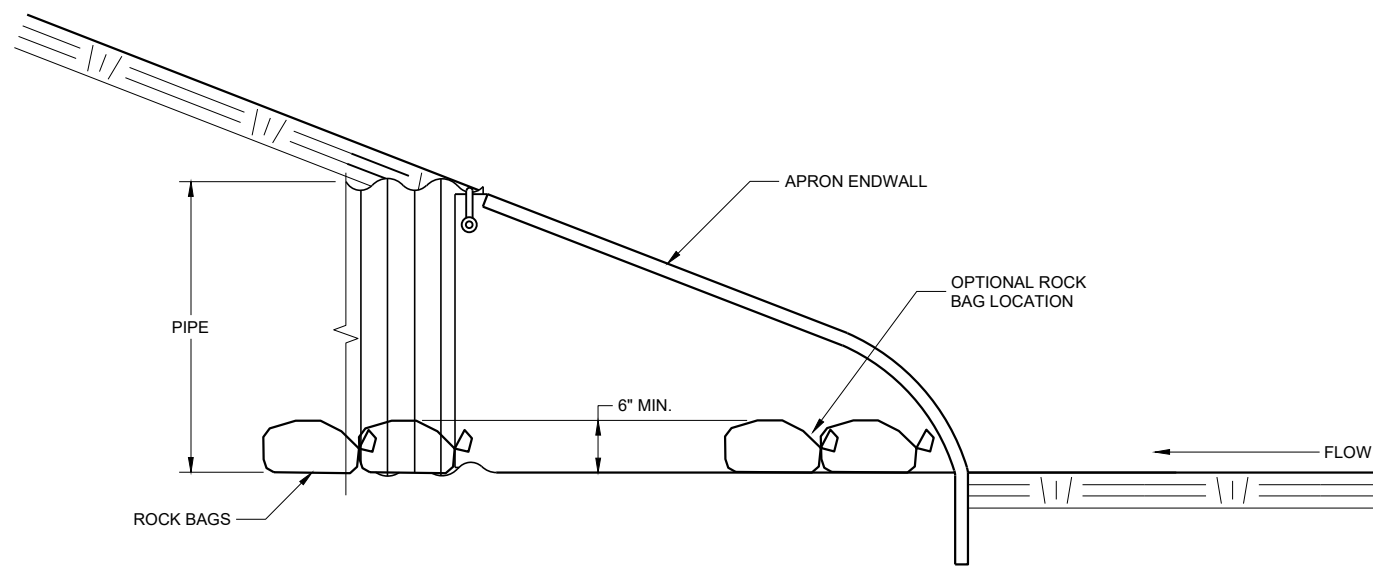
INLET PROTECTION, TYPE D

(CAN BE INSTALLED IN ANY INLET TYPE WITH OR WITHOUT A CURB BOX AS PER NOTE ②)

INLET PROTECTION TYPE A, B, C, AND D	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED 10/16/02 DATE	/s/ Beth Connestra CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA	



END VIEW



SIDE VIEW

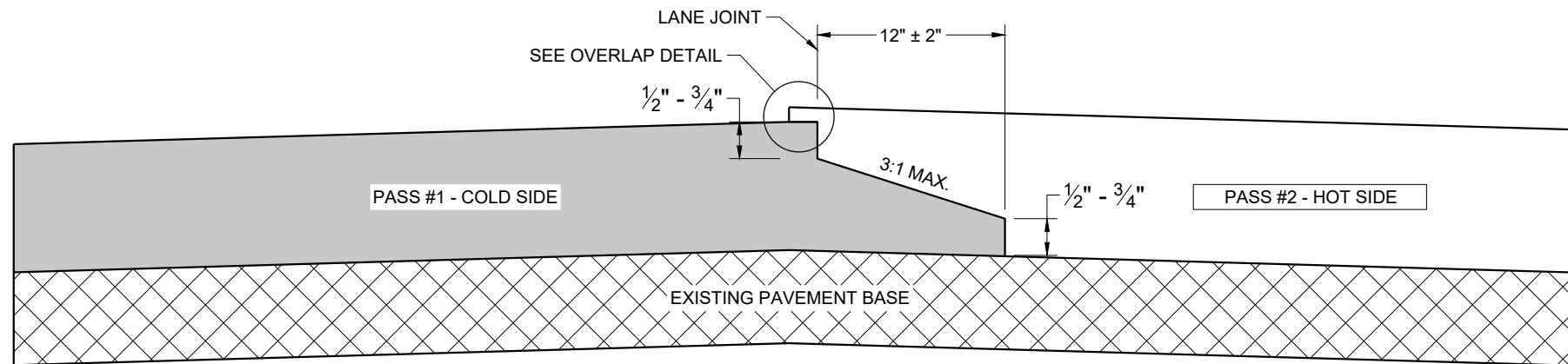
CULVERT PIPE CHECK
 (INSTALL ON INLET END ONLY)

CULVERT PIPE CHECK

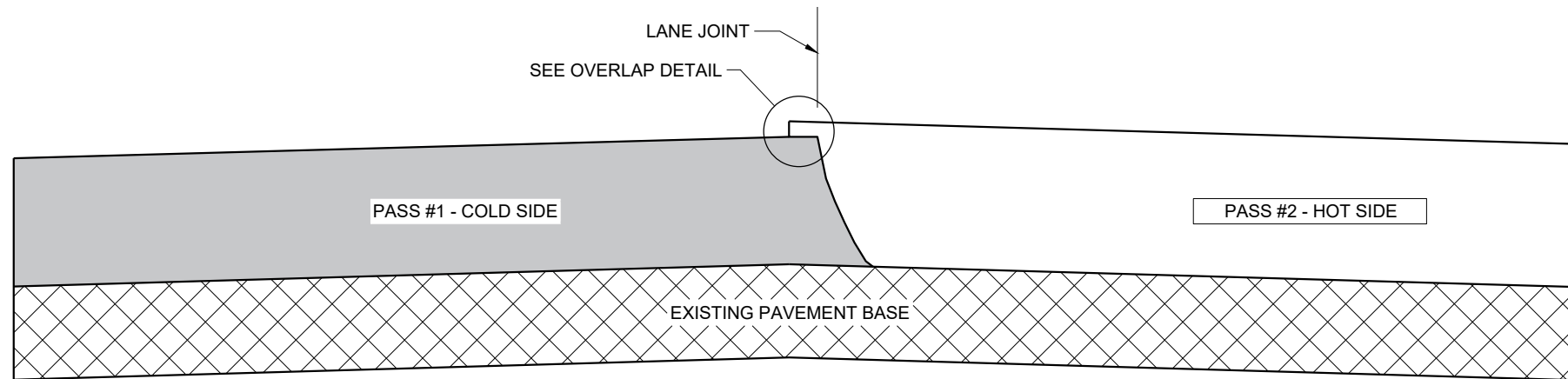
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
 May 2019 /S/ Daniel Schave
 DATE EROSION CONTROL ENGINEER

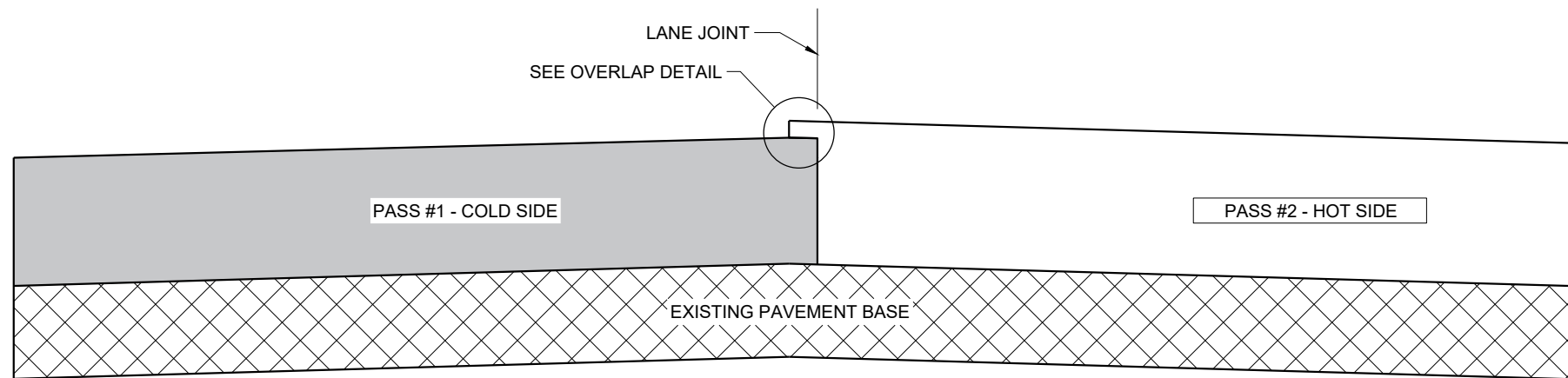
FHWA



**TYPICAL PAVEMENT CROSS SECTION
NOTCHED WEDGE JOINT**



**TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT**



**TYPICAL PAVEMENT CROSS SECTION
VERTICAL JOINT (MILLED)**

GENERAL NOTES

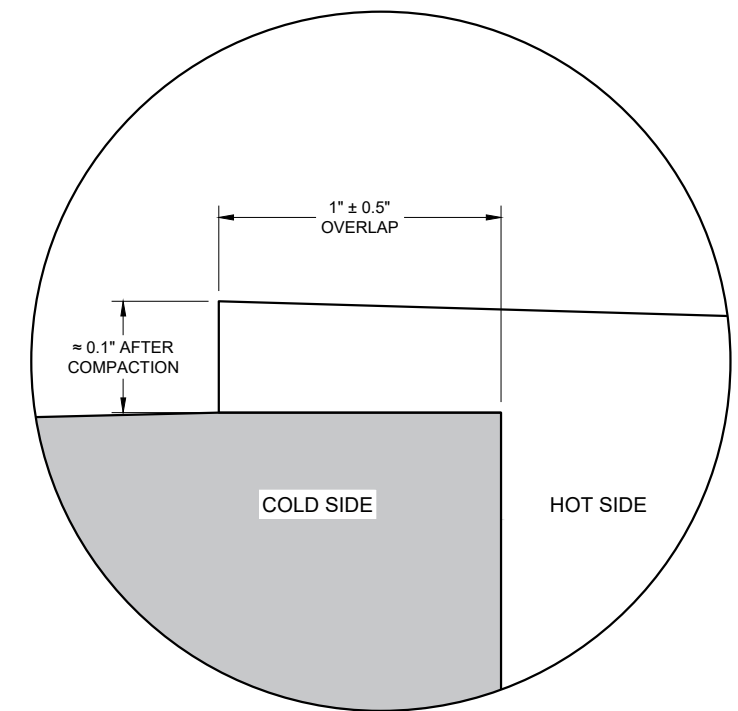
IN ADDITION TO THE DETAILS PROVIDED IN THIS DRAWING, CONFORM TO STANDARD SPECIFICATION 450.3.2.8 FOR WHEN A NOTCHED WEDGE JOINT IS REQUIRED AND FOR GENERAL JOINT CONSTRUCTION REQUIREMENTS.

FOR ALL LONGITUDINAL JOINTS, ENSURE THE PAVER SCREED OVERLAPS THE PREVIOUSLY PLACED PAVEMENT BY $1" \pm 0.5"$ AND THE HOT SIDE OF THE JOINT REMAINS HIGHER THAN THE COLD SIDE BY APPROXIMATELY 0.1" AFTER FINAL COMPACTION. (IT WILL BE FLUSH WHEN PAVING IN ECHELON.)

ONLY REMOVE THE LONGITUDINAL NOTCHED WEDGE JOINT FOR SMA PAVEMENT OR AS DIRECTED BY THE ENGINEER TO ADDRESS SPECIFIC LENGTHS OF JOINT DAMAGED BY TRAFFIC.

WHEN MILLING BACK OR REMOVING ANY LONGITUDINAL JOINT, LIMIT THE MATERIAL REMOVED TO 2" FROM THE TOP NOTCH OR FROM THE VERTICAL JOINT EDGE ON THE COLD SIDE OF THE JOINT.

USE LONGITUDINAL MILLED JOINT AS PLANS SHOW OR THE AS THE ENGINEER DIRECTS.



OVERLAP DETAIL (TYPICAL)

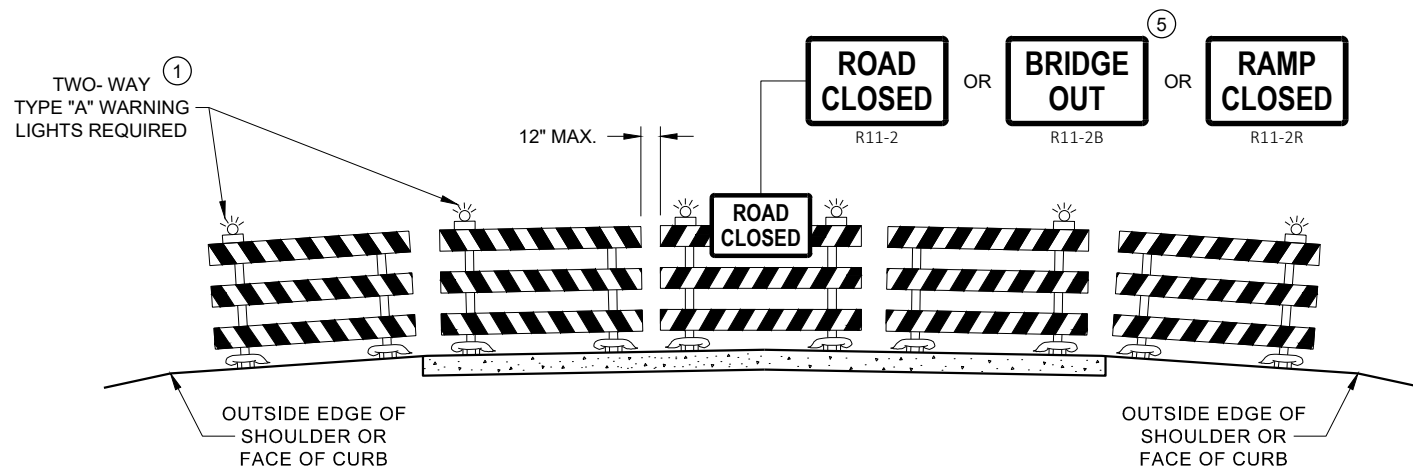
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6

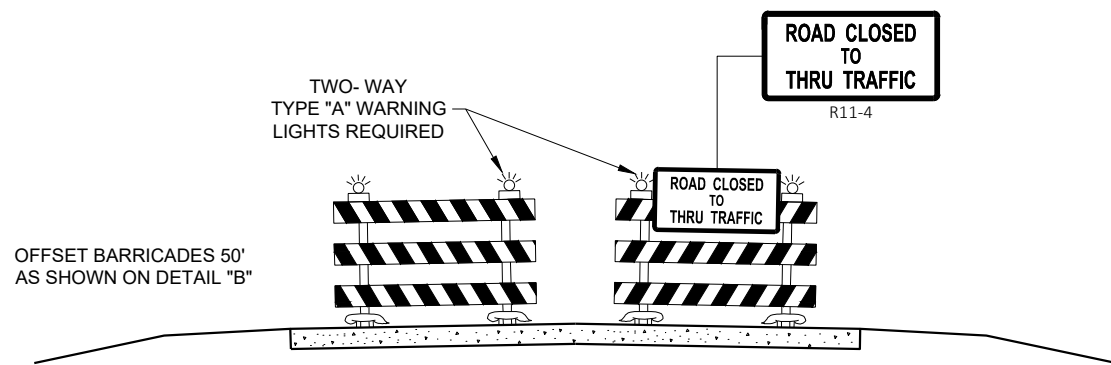
SDD 13C19 - 03

SDD 13C19 - 03

HMA LONGITUDINAL JOINTS	
STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION	
APPROVED November 2020 DATE	/S/ Steven Hefel HMA PAVEMENT ENGINEER
FHWA	



**DETAIL D
ROAD CLOSURE BARRICADE DETAIL
APPROACH VIEW**



**DETAIL E
LANE CLOSURE BARRICADE DETAIL
APPROACH VIEW**

SEE SDD 15C2 - SHEET "a" FOR LEGEND

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE", SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY RE-ESTABLISHED UPON COMPLETION OF THE OPERATION, OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN 7 CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW - INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11 - 2, R11 - 3, M4 - 9, R11 - 4, AND R10 - 61 SIGNS PLACED ON THE BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE RAIL OR BOTTOM RAILS.

"WO" AND "MO" SIGNS ARE THE SAME AS "W" AND "M" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:

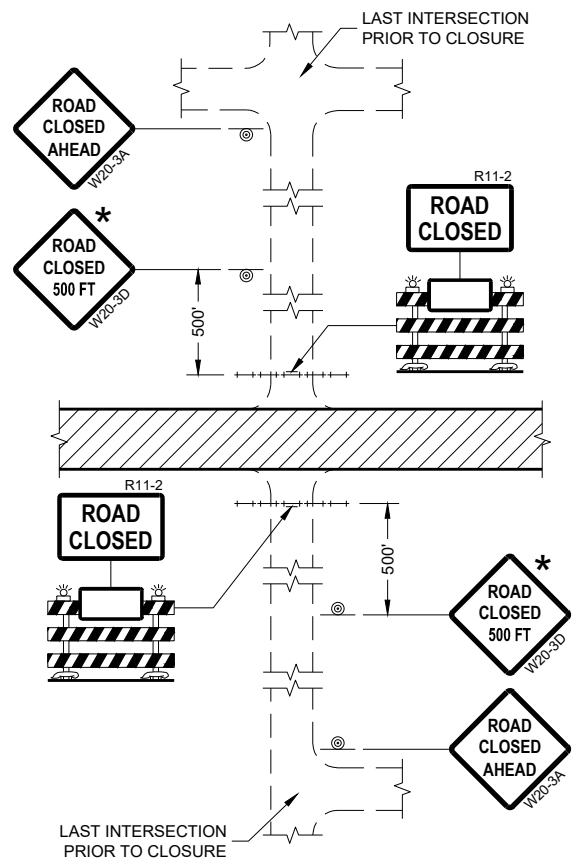
- R11 - 2 SHALL BE 48" X 30"
- R11 - 3 SHALL, R11 - 4 AND R10 - 61 SHALL BE 60" X 30"
- M4 - 9 SHALL BE 30" X 24"
- M3 - X SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M4 - 8 SHALL BE 24" X 12" (36" X 18" IF NEEDED TO MATCH EXISTING SIGNS)
- M1 - 4, M1 - 5A AND M1 - 6 SHALL BE 24" X 24" (36" X 36" IF NEEDED TO MATCH EXISTING SIGNS)
- MO5 - 1 AND MO6 - 1 SHALL BE 21" X 21" (30" X 30" IF NEEDED TO MATCH EXISTING SIGNS)
- D1 - X SHALL BE AS SHOWN ON SPECIFIC PROJECT SIGNING DETAIL SHEETS.
- R1 - 1 SHALL BE 36" X 36"

- ① TWO WARNING LIGHTS SHALL BE PROVIDED ON THE CENTER BARRICADE AND A MINIMUM OF ONE WARNING LIGHT SHALL BE PROVIDED ON EACH OF THE OTHER BARRICADES WITHIN THE ROADWAY LIMITS. SPACING OF THE WARNING LIGHTS SHALL BE UNIFORM TO THE EDGE OF ROADWAY AS SHOWN (APPROX. 8 FOOT LIGHT SPACING).
- ② THESE SIGNS AND BARRICADES ARE NOT REQUIRED IF ROAD CLOSURE BEGINS AT AN INTERSECTION.
- ③ FOR ROAD CLOSURE WITHOUT LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "D".
- ④ FOR ROAD CLOSURE WITH LOCAL ACCESS TO PROJECT, SEE ROAD CLOSURE BARRICADE DETAIL "E".
- ⑤ FOR BRIDGE OR CULVERT REPLACEMENTS, SUBSTITUTE "BRIDGE OUT" INSTEAD OF "ROAD CLOSED" ON R11 - 2 AND R11 - 3 SIGNS.
- ⑥ INSTALL DETOUR AND COMMUNITY GUIDE SIGNS AND ARROWS ONLY IF SPECIFIED IN THE CONTRACT. IF THERE ARE EXISTING ROUTE MARKER ASSEMBLIES THAT WILL REMAIN IN PLACE, ADJUST THE LOCATION OF THE DETOUR ROUTE SIGNS TO CORRESPOND WITH THE EXISTING ASSEMBLIES. MODIFY EXISTING SIGNS WHERE POSSIBLE. SEE SPECIFIC PROJECT DETOUR SIGNING DETAIL SHEETS. IF DETOUR SIGNS ARE BEING INSTALLED BY OTHERS, PLACE THE CONTRACTED TRAFFIC CONTROL SIGNS TO ALLOW FOR PLACEMENT OF ALL WARNING, DETOUR AND GUIDE SIGNS AS SHOWN.
- ⑦ "EAST" CARDINAL DIRECTION MARKERS AND RIGHT TURN ARROWS ARE SHOWN. USE OTHER CARDINAL DIRECTIONS AND ARROWS AS APPROPRIATE.

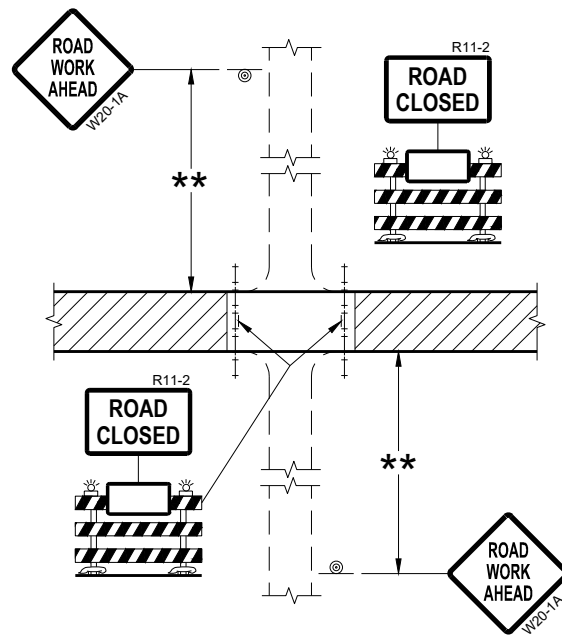
**BARRICADES AND SIGNS
FOR
VARIOUS CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

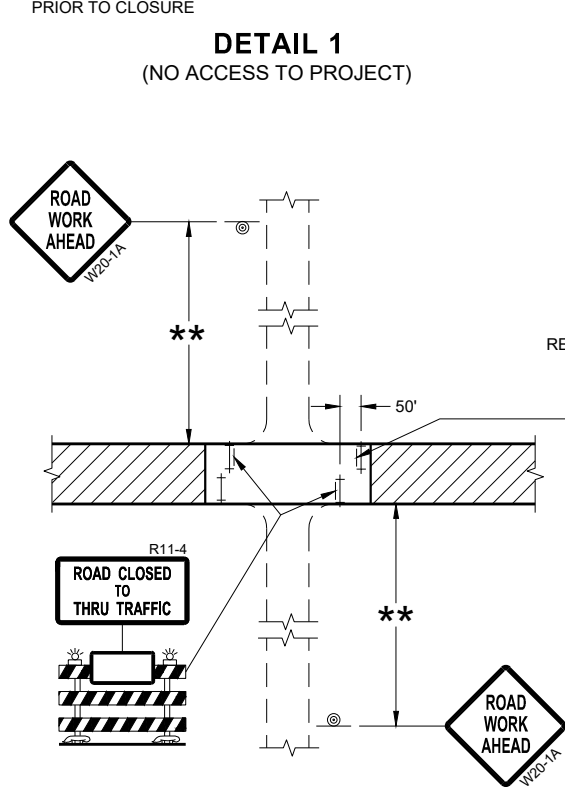
APPROVED
May 2023 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER



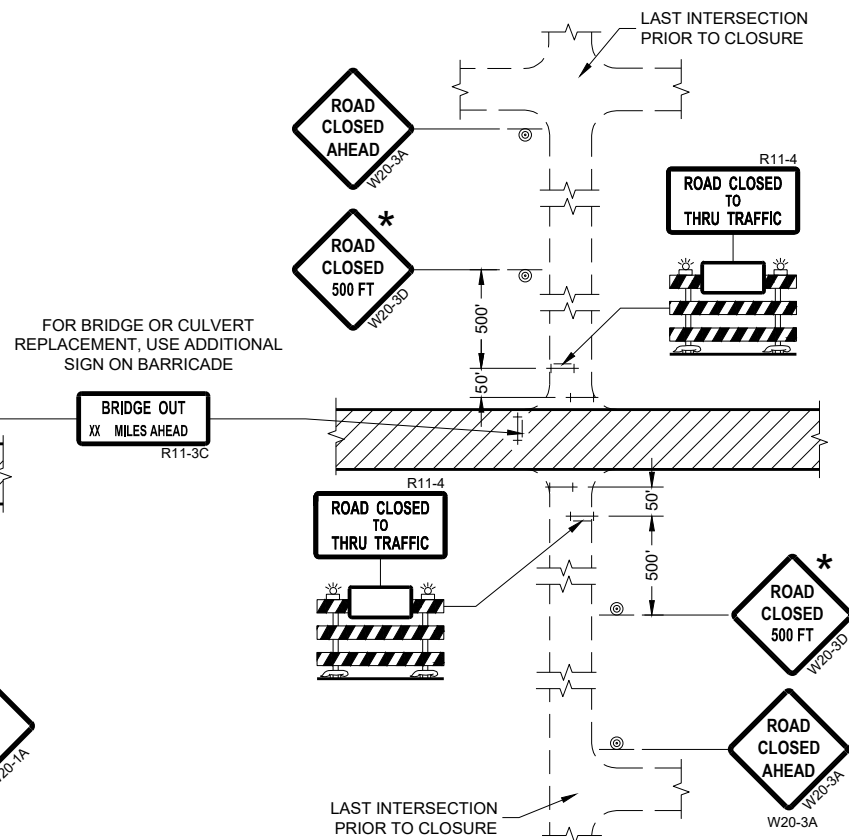
DETAIL 1
(NO ACCESS TO PROJECT)



DETAIL 2
(PUBLIC CROSS-TRAFFIC MAINTAINED.
NO ACCESS TO PROJECT)



DETAIL 3
(PUBLIC CROSS-TRAFFIC MAINTAINED.
CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)



DETAIL 4
(CONTRACTOR, LOCAL BUSINESS AND
RESIDENT ACCESS TO PROJECT)

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND BARRICADES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A DESIRABLE MINIMUM OF 200 FEET CLEARANCE (500 FEET DESIRABLE) TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

BARRICADES THAT MUST BE MOVED FOR A WORK OPERATION SHALL BE IMMEDIATELY REESTABLISHED UPON COMPLETION OF THE OPERATION OR FOR CONTINUING OPERATIONS, AT THE END OF EACH WORKING DAY.

SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

ALL TYPE III BARRICADES SHALL HAVE RAILS REFLECTORIZED ON BOTH FACES. STRIPES SHALL BE PROPERLY SLOPED DOWN TOWARD THE TRAFFIC SIDE OR AS SHOWN IN THE ROAD CLOSURE BARRICADE DETAIL "D" FOR FULL ROAD CLOSURES.

TYPE "A" LOW-INTENSITY FLASHING WARNING LIGHTS SHALL BE VISIBLE ON BOTH SIDES OF THE BARRICADE.

THE R11-2, R11-3, AND R11-4 SIGNS PLACED ON BARRICADES SHALL COVER NO MORE THAN THE TOP RAIL. THE SIGNS SHALL NOT COVER ANY PORTION OF THE MIDDLE OR BOTTOM RAILS.

ALL SIGNS SHALL BE 48" X 48" UNLESS OTHERWISE NOTED BELOW:
R11-2 SHALL BE 48" X 30".
R11-4 AND R11-3 SHALL BE 60" X 30".

- * OMIT THE "ROAD CLOSED 500 FT." SIGN IF THE LAST INTERSECTION IS 500 FEET OR LESS FROM THE WORK ZONE.
- ** 500' MAX. OR AT LAST INTERSECTION, WHICHEVER IS CLOSEST.

LEGEND

- SIGN ON PERMANENT SUPPORT
- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TYPE "A" WARNING LIGHT (FLASHING)
- WORK AREA

**BARRICADES AND SIGNS
FOR
SIDEROAD CLOSURES**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
July 2018 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER

GENERAL NOTES

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS.

THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND SHOULD PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE.




ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED. IF NECESSARY DUE TO SPACE CONSTRAINTS, 36"X36" SIGNS MAY BE USED INSTEAD OF 48" X 48" SIGNS.

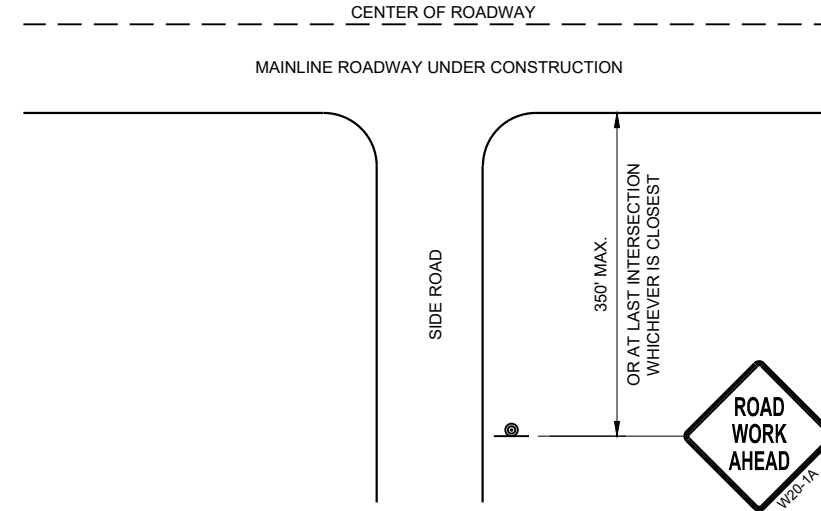
SIGNS THAT WILL BE IN PLACE LESS THAN SEVEN CONTINUOUS DAYS AND NIGHTS MAY BE MOUNTED ON PORTABLE SUPPORTS.

IF A "STOP" SIGN MUST BE REMOVED FOR A WORK OPERATION, A TEMPORARY "STOP" SIGN SHALL BE PLACED PRIOR TO THE SIGN REMOVAL, OR A FLAGGER SHALL BE PROVIDED UNTIL THE SIGN IS REESTABLISHED.

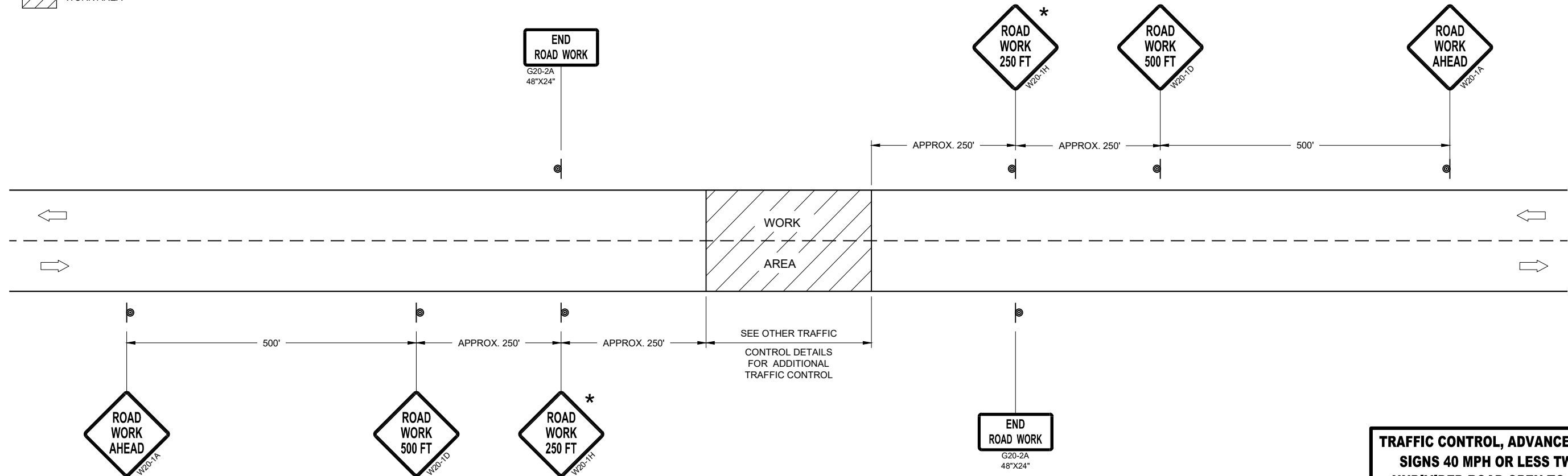
* THE THIRD W20-1 SIGN IS REQUIRED ONLY IF THERE IS AN INTERSECTION BETWEEN THE "ROAD WORK 500 FEET" SIGN AND THE WORK ZONE. ADJUST THE PLACEMENT OF THIS SIGN BASED ON INTERSECTION LOCATION AND OTHER FIELD CONDITIONS.

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  DIRECTION OF TRAFFIC
-  WORK AREA



**TYPICAL SIDE ROAD APPROACH
WARNING SIGN DETAIL**



TRAFFIC CONTROL, ADVANCE WARNING SIGNS 40MPH OR LESS






**TRAFFIC CONTROL, ADVANCE WARNING
SIGNS 40 MPH OR LESS TWO-WAY
UNDIVIDED ROAD OPEN TO TRAFFIC**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE July 2018 /S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

LEGEND

-  SIGN ON PORTABLE OR PERMANENT SUPPORT
-  TEMPORARY PORTABLE RUMBLE STRIP ARRAY
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS, DEVICES, AND LOCATION OF ALL FLAGGERS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGING

FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT REMOVE TEMPORARY PORTABLE RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.

- ① FOR MOVING WORK OPERATIONS, POST ADDITIONAL W20-7A FLAGGER SIGNS AT APPROXIMATELY 3,500' INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.
- ② SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

WHEN THE DISTANCE BETWEEN FLAGGERS EXCEEDS 2 MILES, A PILOT CAR IS REQUIRED. WHEN CURVES REDUCE SIGHT DISTANCE BELOW 400', A PILOT CAR IS REQUIRED.

TEMPORARY PORTABLE RUMBLE STRIPS

UTILIZE TEMPORARY PORTABLE RUMBLE STRIPS ON ALL FLAGGING OPERATIONS.

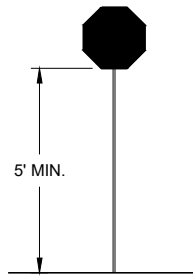
- ③ EACH TEMPORARY PORTABLE RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS PLACED TRANSVERSE ACROSS THE LANE AT THE LOCATIONS SHOWN. WITHIN EACH ARRAY, SPACING BETWEEN RUMBLE STRIPS SHALL BE 15 FEET ON CENTER

ONLY USE TEMPORARY PORTABLE RUMBLE STRIPS FROM THE APPROVED PRODUCTS LIST.

INSTALL TEMPORARY RUMBLE STRIPS PER MANUFACTURER'S RECOMMENDATIONS.

PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.

DO NOT INSTALL TEMPORARY PORTABLE RUMBLE STRIPS ON GRAVEL, MILLED SURFACES, OR ASPHALT THAT HAS BEEN PAVED LESS THAN 12 HOURS.



STOP/SLOW PADDLE ON SUPPORT STAFF

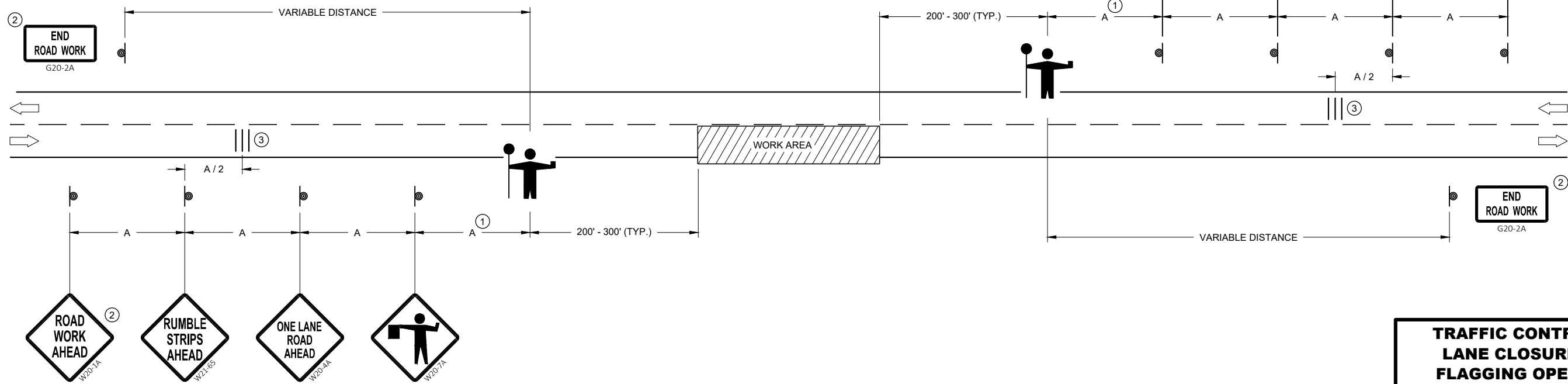
SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE

SPEED LIMIT	SPACING "A"
25-30 MPH	200'
35-40 MPH	350'
45-55 MPH	500'



W03-4

USE OF W03-4 SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7A AND W20-4A SIGNS, USING SPACING "A".



TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
DATE: May 2022 /S/ Andrew Heidtke
WORK ZONE ENGINEER

FHWA

Notes



Wisconsin Department of Transportation

Dedicated people creating transportation solutions through innovation and exceptional service.

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