

WKE JUNE 2024

| STATE PROJECT | FEDERAL PROJECT | |
|---------------|-----------------|----------|
| | PROJECT | CONTRACT |
| 3846-00-75 | WISC 2024354 | 1 |
| | | |
| | | |
| | | |

STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED IMPROVEMENT

T-LINN LACKEY LANE

OVER LAKE GENEVA TRIBUTARY P-64-0906

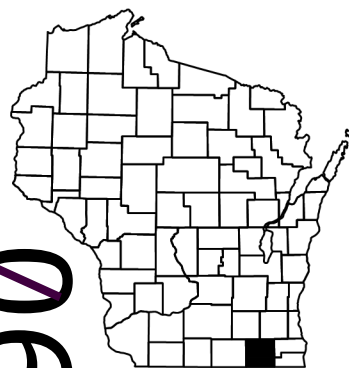
**LOCAL STREET
WALWORTH COUNTY**

| |
|----------------------|
| STATE PROJECT NUMBER |
| 3846-00-75 |

ORDER OF SHEETS

| Section No. | Title |
|--------------|------------------------------|
| 1 | Title |
| 2 | Typical Sections and Details |
| 3 | Estimate of Quantities |
| 3 | Miscellaneous Quantities |
| 4 | Right of Way Plat |
| 5 | Plan and Profile |
| 6 | Standard Detail Drawings |
| 7 | Sign Plates |
| 8 | Structure Plans |
| 9 | Computer Earthwork Data |
| 9 | Cross Sections |

TOTAL SHEETS = 64



06

DESIGN DESIGNATION

| | | | |
|--------------|------|---|--------|
| A.A.D.T. | 2024 | = | 44 |
| A.A.D.T. | 2044 | = | 50 |
| D.H.V. | | = | 5 |
| D.D. | | = | 50/50 |
| T. | | = | 2% |
| DESIGN SPEED | | = | 25 MPH |
| ESALS | | = | |

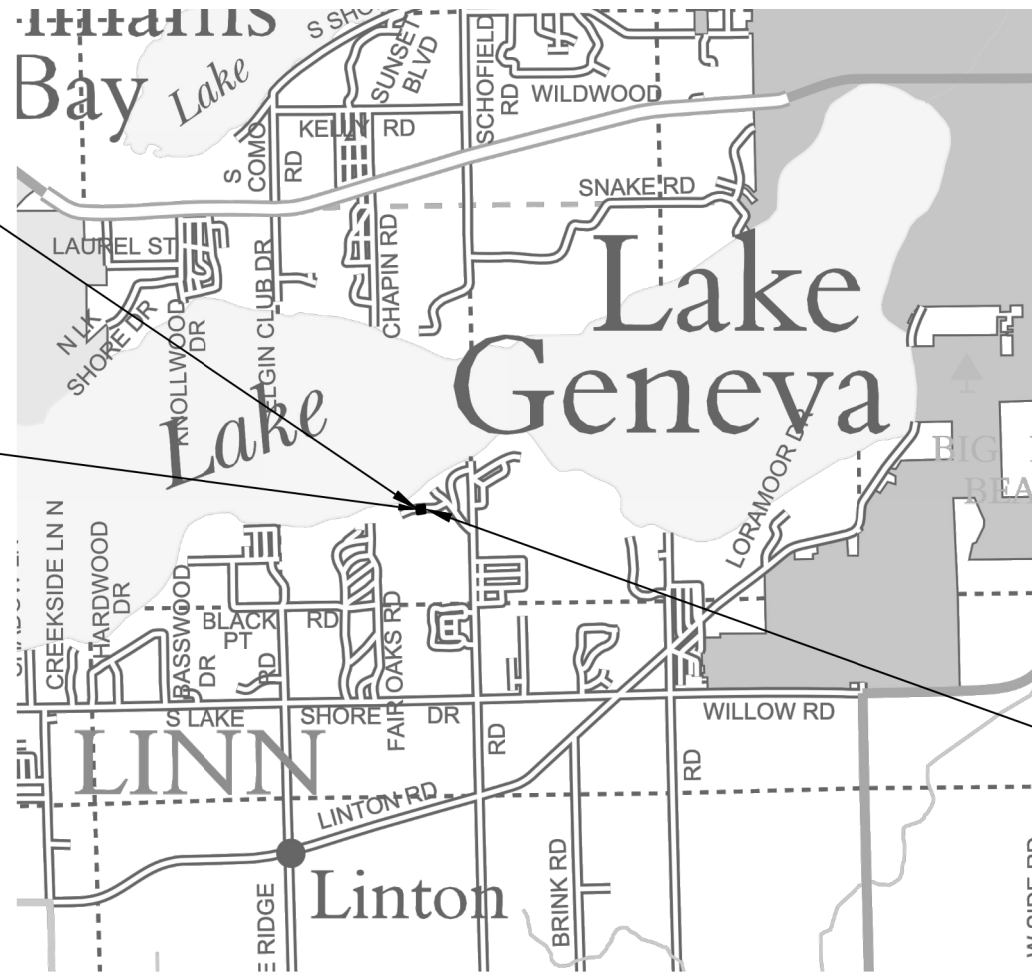
CONVENTIONAL SYMBOLS

| PLAN | PROFILE |
|-----------------------------------|------------------------------------------------|
| CORPORATE LIMITS | GRADE LINE |
| PROPERTY LINE | ORIGINAL GROUND |
| LOT LINE | MARSH OR ROCK PROFILE (To be noted as such) |
| LIMITED HIGHWAY EASEMENT | SPECIAL DITCH |
| EXISTING RIGHT OF WAY | GRADE ELEVATION |
| PROPOSED OR NEW R/W LINE | CULVERT (Profile View) |
| SLOPE INTERCEPT | UTILITIES |
| REFERENCE LINE | ELECTRIC |
| EXISTING CULVERT | FIBER OPTIC |
| PROPOSED CULVERT (Box or Pipe) | GAS |
| COMBUSTIBLE FLUIDS | SANITARY SEWER |
| MARSH AREA | STORM SEWER |
| WOODED OR SHRUB AREA | TELEPHONE |
| | WATER |
| | UTILITY PEDESTAL |
| | POWER POLE |
| | TELEPHONE POLE |

BEGIN PROJECT
STA 20+32
Y = 324587.236
X = 777364.599

STRUCTURE P-64-0906
TO BE REMOVED
STA 21+98.98

END PROJECT
STA 23+20
X = 324607.600
Y = 777651.446



TOTAL NET LENGTH OF CENTERLINE = 0.055 MILES

HORIZONTAL POSITION SHOWN ON THIS PLAN ARE WISCONSIN COUNTY COORDINATE REFERENCE SYSTEM (LM-WCCS), WALWORTH COUNTY, NAD83 (2011) U.S. SURVEY FEET.

ELEVATIONS ARE REFERENCED TO NAVD 88 (2007) VERTICAL DATUM

ACCEPTED FOR
TOWN OF LINN

DocuSigned by:
James Weiss

Date: 3/14/2024

ORIGINAL PLANS PREPARED BY
BAXTER & WOODMAN
Consulting Engineers

DATE: 3/15/24
David Hemmerich
(Professional Engineer Signature)

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

PREPARED BY

| | |
|---------------------|-----------------------|
| Surveyor | BAXTER & WOODMAN, INC |
| Designer | BAXTER & WOODMAN, INC |
| Project Manager | JOSEPH JELACIC, PE |
| Regional Examiner | |
| Regional Supervisor | BRIAN BOOTHBY |

APPROVED FOR THE DEPARTMENT

Digitally signed by Joseph Jelacic
DN: cn=Joseph Jelacic, o=Wisconsin
Department of Transportation,
Date: 2024.03.14 14:59:05-0500

Date: 3/14/2024
Joseph Jelacic
(Signature)

E

GENERAL NOTES

THE LOCATIONS OF EXISTING AND PROPOSED UTILITY INSTALLATIONS AS SHOWN ON THE PLANS ARE APPROXIMATE. THERE MAY BE OTHER UTILITY INSTALLATIONS WITHIN THE PROJECT AREA THAT ARE NOT SHOWN.

THE CONTRACTOR SHALL NOTIFY DIGGERS HOTLINE AND AFFECTED UTILITIES PRIOR TO THE START OF CONSTRUCTION.

RIGHT OF WAY INFORMATION SHOWN ON THE PLANS IS APPROXIMATE.

NO TREES OR SHRUBS ARE TO BE REMOVED WITHOUT THE APPROVAL OF THE ENGINEER.

TRAFFIC CONTROL DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.

DO NOT DRIVE OR STORE EQUIPMENT, OR STORE CONSTRUCTION MATERIALS IN ENVIRONMENTALLY SENSITIVE AREAS, WETLANDS OR WATERWAYS.

DO NOT STORE EQUIPMENT OR CONSTRUCTION MATERIALS WHERE THEY WILL OBSTRUCT STREET CROSSINGS OR DRIVEWAY SIGHTLINES.

ACCESS TO PRIVATE DRIVEWAYS SHALL BE PROVIDED AT ALL TIMES EXCEPT DURING ACTUAL CONSTRUCTION ADJACENT THERE TO.

IT WILL BE THE RESPONSIBILITY OF THE CONTRACTOR TO NOTIFY RESIDENTS WHEN ACCESS TO DRIVEWAYS WILL BE TEMPORARILY CLOSED DUE TO CONSTRUCTION ACTIVITIES. THE CONTRACTOR SHALL DISTRIBUTE NOTICES AT LEAST 24 HOURS PRIOR TO PLANNED CLOSURE. EVERY EFFORT SHALL BE MADE TO ACCOMMODATE ACCESS TO THESE PROPERTIES.

STANDARD ABBREVIATIONS

| | | | |
|------|-------------------------------|--------|--------------------------|
| ABUT | ABUTMENT | INTERS | INTERSECTION |
| AP | ACCESS POINT | INV | INVERT |
| AGG | AGGREGATE | JT | JOINT |
| ASPH | ASPHALTIC | LT | LEFT |
| BL | BASELINE | LF | LINEAR FOOT |
| BM | BENCHMARK | MH | MANHOLE |
| CB | CATCH BASIN | MP | MARKER POST |
| CL | CENTER LINE | MB | MESSAGE BOARD |
| CONC | CONCRETE | NOM | NOMINAL |
| CO | COUNTY | NB | NORTHBOUND |
| CABC | CRUSHED AGGREGATE BASE COURSE | PAVT | PAVEMENT |
| CY | CUBIC YARD | PU | PIPE UNDERDRAIN |
| CULV | CULVERT | PCC | PORTLAND CEMENT CONCRETE |
| CP | CULVERT PIPE | PE | PRIVATE ENTRANCE |
| C&G | CURB AND GUTTER | PROJ | PROJECT |
| DIA | DIAMETER | PL | PROPERTY LINE |
| DWY | DRIVEWAY | RL | REFERENCE LINE |
| EB | EASTBOUND | RT | RIGHT |
| ELEV | ELEVATION | R/W | RIGHT OF WAY |
| EW | ENDWALL | RDWY | ROADWAY |
| ENT | ENTRANCE | SHLDR | SHOULDER |
| EXC | EXCAVATION | SB | SOUTHBOUND |
| FP | FENCE POST | SS | STORM SEWER |
| FERT | FERTILIZE | TEL | TELEPHONE |
| F | FILL | TEMP | TEMPORARY |
| FG | FINISHED GRADE | TER | TERRACE |
| FL | FLOW LINE | TV | TELEVISION |
| FO | FIBER OPTIC | UG | UNDERGROUND |
| FT | FOOT | VOL | VOLUME |
| HYD | HYDRANT | W | WATER |
| INL | INLET | WB | WESTBOUND |

UTILITY CONTACTS

ALLIANT ENERGY - ELECTRICITY
ATTN: AUSTIN KROHN
400B KOOPMAN LANE
ELKHORN, WI 53121
PHONE: 262-741-0905
EMAIL: AUSTINKROHN@ALLIANTENERGY.COM

AT&T DISTRIBUTION - COMMUNICATION LINE
ATTN: NATE GIBERT
411 7TH STREET
RACINE, WI 53403
PHONE: 262-720-8235
EMAIL: NG952W@ATT.COM

CHARTER COMMUNICATIONS - COMMUNICATION LINE
ATTN: CODY THOMPSON
1348 PLAINFIELD AVENUE
JANESVILLE, WI 535.45
PHONE: 608-921-8282
EMAIL: CODY.THOMPSON@CHARTER.COM

WE ENERGIES - GAS
ATTN: SCOTT HOLSTEIN
700 S. KANE STREET
BURLINGTON, WI 53105
PHONE: 262-763-1084
EMAIL: WE-UTILITY-RELOCATIONS@WE-ENERGIES.COM

OTHER CONTACTS

WISDOT CONTACT
LOCAL PROGRAM PROJECT MANAGER
ATTN: JOSEPH JELACIC, PE
141 NW BARSTOW STREET
PO BOX 798
WAUKESHA, WI 53188
PHONE: 262-548-6762
EMAIL: JOSEPH.JELACIC@DOT.WI.GOV

DESIGN CONTACT
BAXTER & WOODMAN, INC.
ATTN: DAVID HEMMERICH, PE
256 S. PINE STREET
BURLINGTON, WI 53105
PHONE: 815-444-3207
EMAIL: DHEMMERICH@BAXTERWOODMAN.COM

DNR LIAISON
ATTN: CRAIG WEBSTER
141 NW BARSTOW STREET
WAUKESHA, WI 53188
PHONE: 262-574-2141
EMAIL: CRAIG.WEBSTER@WISCONSIN.GOV5

TOWN OF LINN
HIGHWAY SUPERINTENDENT
ATTN: MATT WITTUM
W3728 FRANKLIN WALSH ST,
ZENDA, WI 53195
PHONE: 262-275-6300 EXT. 17
EMAIL: linnhighway@townoflinn.wi.gov

INDEX OF SECTION 2 SHEETS

- GENERAL NOTES
- PROJECT OVERVIEW
- TYPICAL SECTIONS
- REMOVAL PLAN
- CONTOUR MAP
- EROSION CONTROL
- TRAFFIC CONTROL
- ALIGNMENT DETAILS



Dial **811** or (800)242-8511

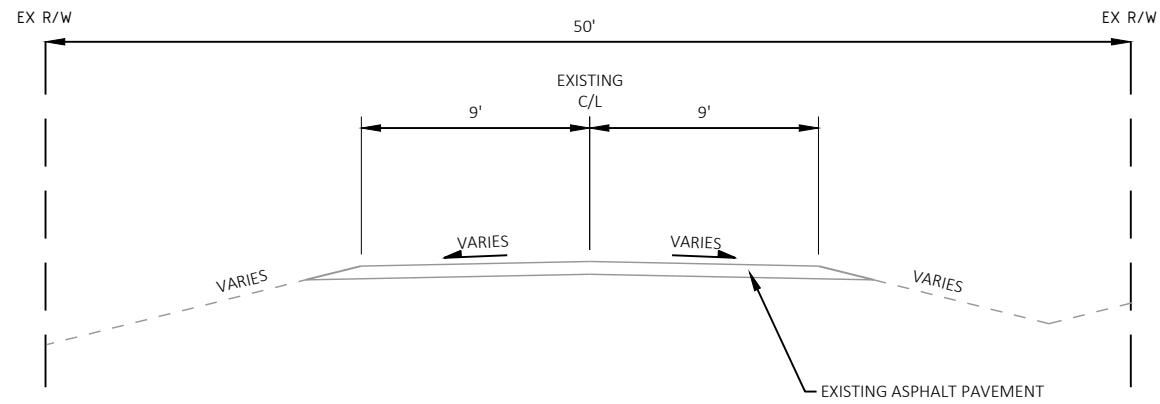
www.DiggersHotline.com



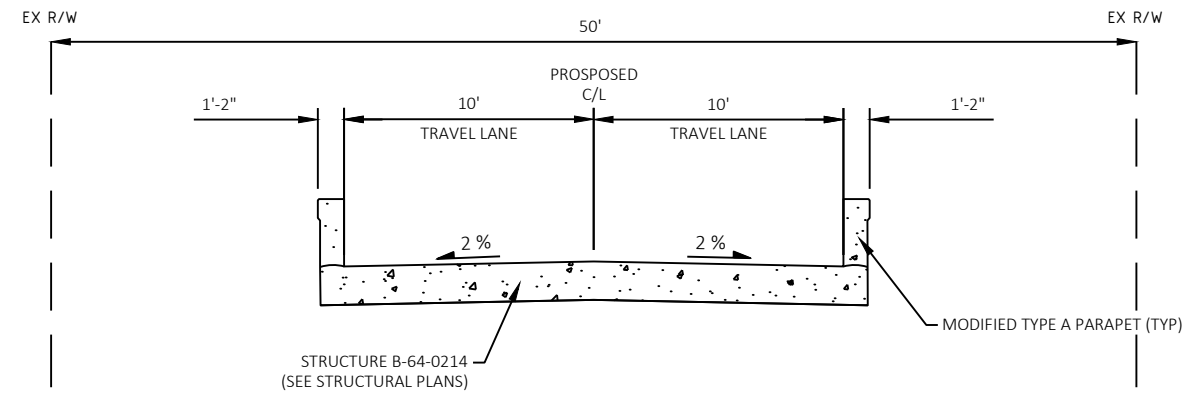
BEGIN PROJECT
 STA 20+32
 Y = 324587.236
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 X = 324607.600
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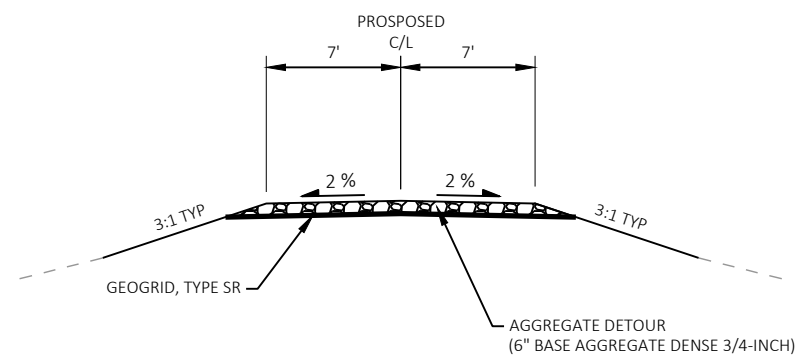
PROPOSED
 STRUCTURE B-64-214



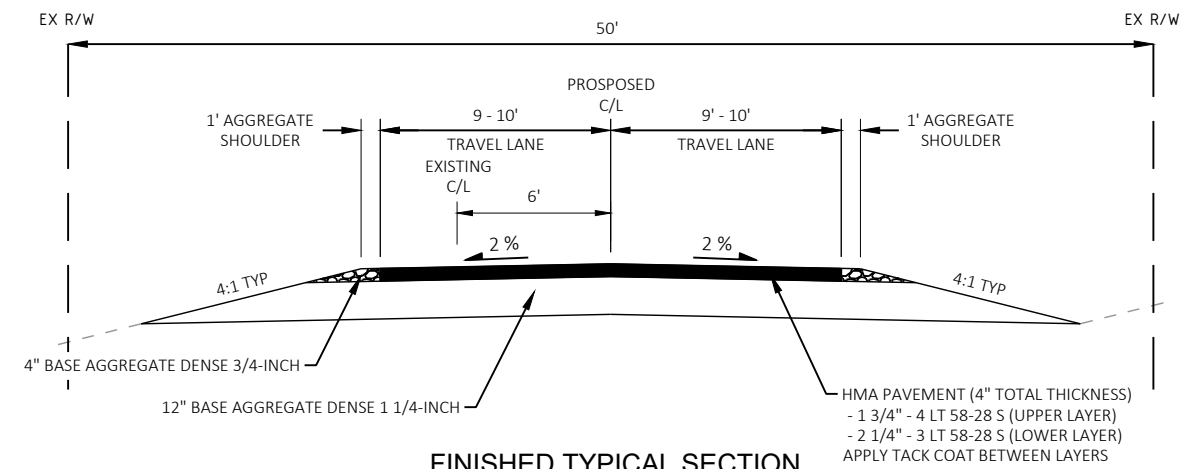
EXISTING TYPICAL SECTION
STA 20+32 TO STA 23+20



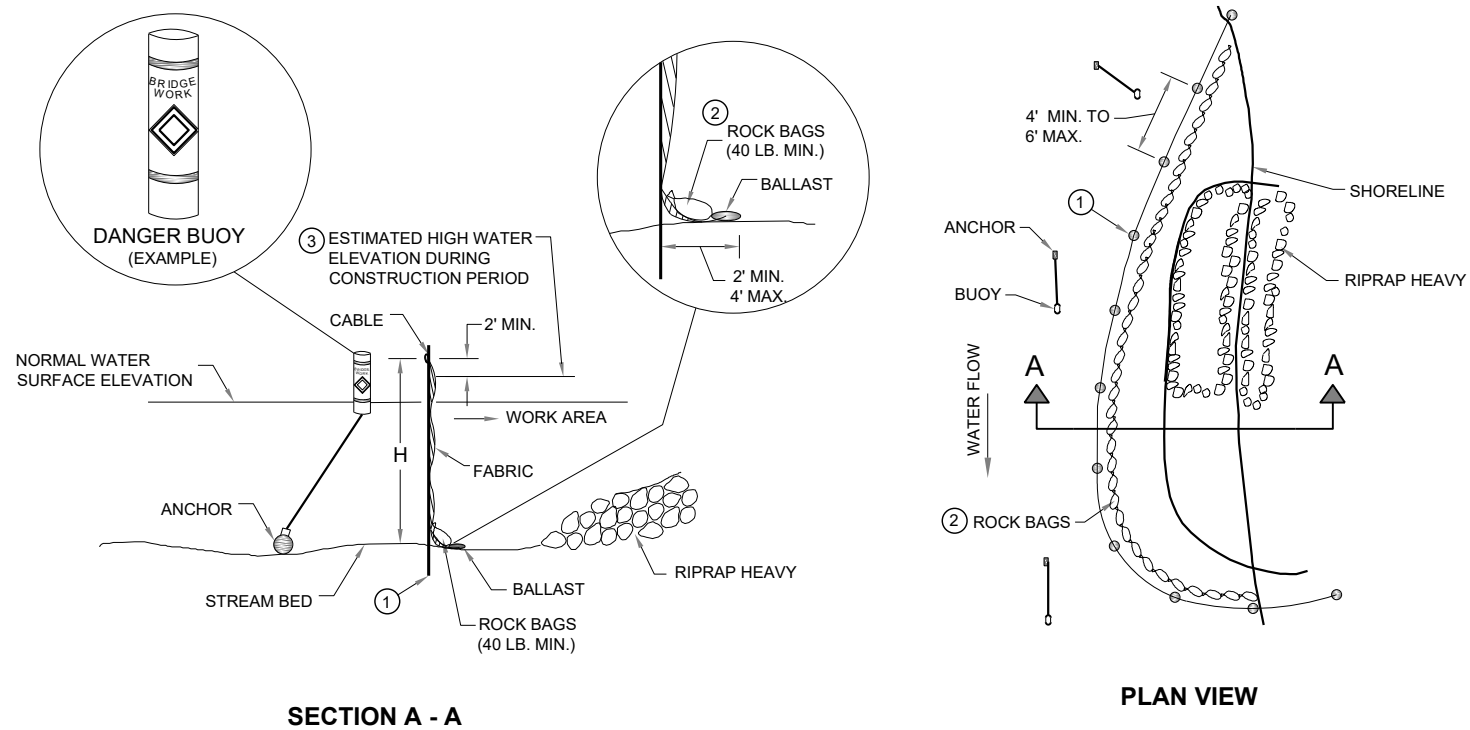
FINISHED TYPICAL SECTION
STA 21+84 TO STA 22+16



TYPICAL SECTION - TEMPORARY BYPASS
STA 40+25 TO STA 43+25



FINISHED TYPICAL SECTION
STA 20+32 TO STA 21+84
STA 22+16 TO STA 23+20



SECTION A - A

PLAN VIEW

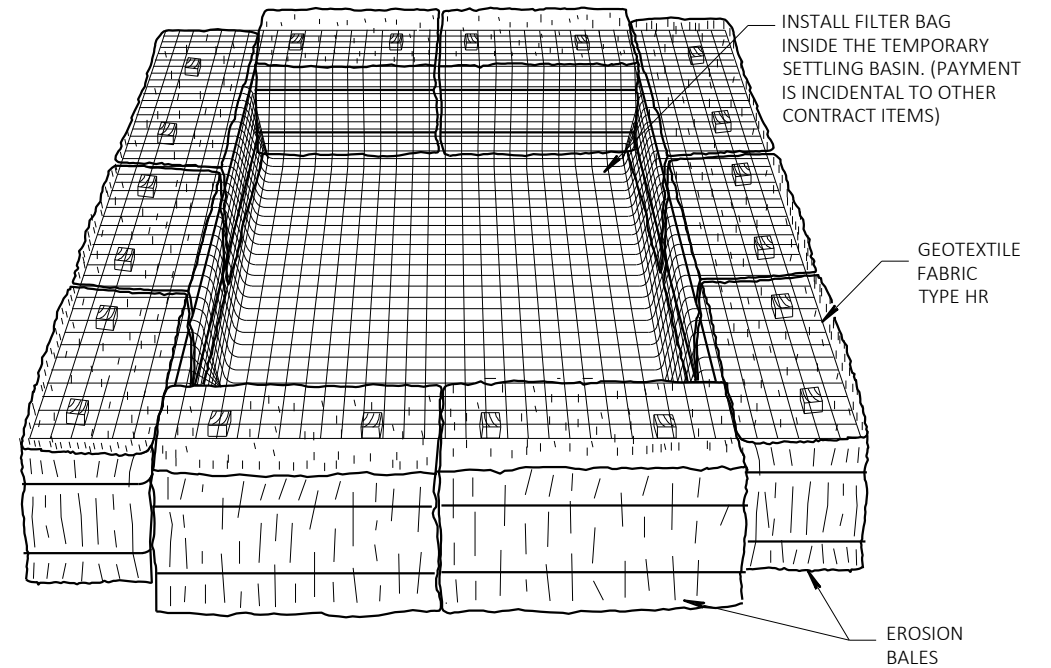
NOTES:

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS. SEE SDD 08E11-02 TURBIDITY BARRIER FOR ADDITIONAL INFORMATION.

TURBIDITY BARRIER MAY BE REMOVED AT THE ENGINEER'S DISCRETION, WHEN PERMANENT EROSION CONTROL MEASURES HAVE BEEN ESTABLISHED.

- ① DRIVEN STEEL POSTS, PIPES, OR CHANNELS, LENGTH SHALL BE SUFFICIENT TO SECURELY SUPPORT BARRIER AT HIGH WATER ELEVATIONS.
- ② INSTALL A CONTINUOUS LINE OF ROCK BAGS TO ANCHOR THE BARRIER TO THE STREAM BED.
- ③ ESTIMATE HIGH WATER ELEVATION DURING CONSTRUCTION PERIOD. MINIMUM BARRIER HEIGHT SHALL BE 2 FEET GREATER THAN EITHER THE Q2 ELEVATION OR THE ESTIMATED HIGH WATER ELEVATION DURING CONSTRUCTION, WHICHEVER IS GREATER.

ENHANCED TURBIDITY BARRIER DETAIL
NOT TO SCALE



WATER PUMPED FROM THE EXCAVATION SHALL BE TREATED FOR WATER QUALITY PRIOR TO DISCHARGE FROM THE SITE. FILTER BAGS AND PUMPS SHALL NOT BE PLACED IN WETLANDS OR ON SOILS THAT ARE BARE, ERODIBLE OR UPGRADE FROM THOSE SOILS.

THE FILTER BAG SHALL BE PLACED IN A EROSION BALE LINED BOX AND SHALL BE DOWN-GRADIENT OF BARE SOILS.

WHEN THE FILTER BAG BECOMES FILLED WITH SEDIMENT IT SHALL BE PROPERLY REMOVED AND DISPOSED.

FILTER BAGS SHALL BE SECURELY ATTACHED TO THE DISCHARGE PIPE.

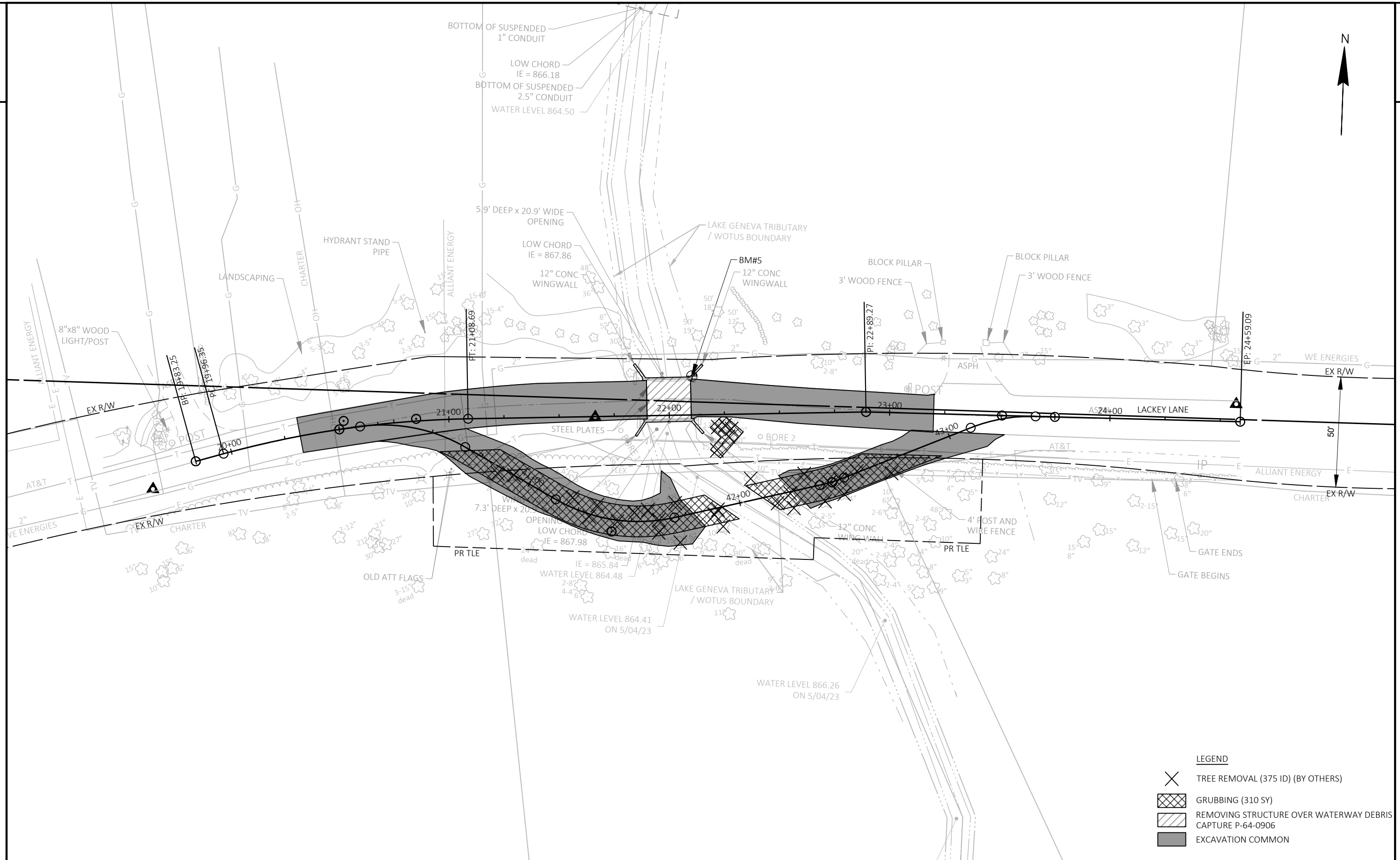
STORAGE VOLUME (C.F.) = 16 X GPM (PUMP RATE)

EXAMPLE:
CONTRACTOR INDICATES PUMP CAPABLE OF 50 GPM
HEIGHT OF BALES = 1.5 FT.





SOLUTION:
SV (C.F.) = 16 X 50
SV = 800 C.F.
 $\frac{800 \text{ C.F.}}{1.5 \text{ FT.}} = 533 \text{ S.F.}$

USE A 20 FT. X 27 FT. BASIN

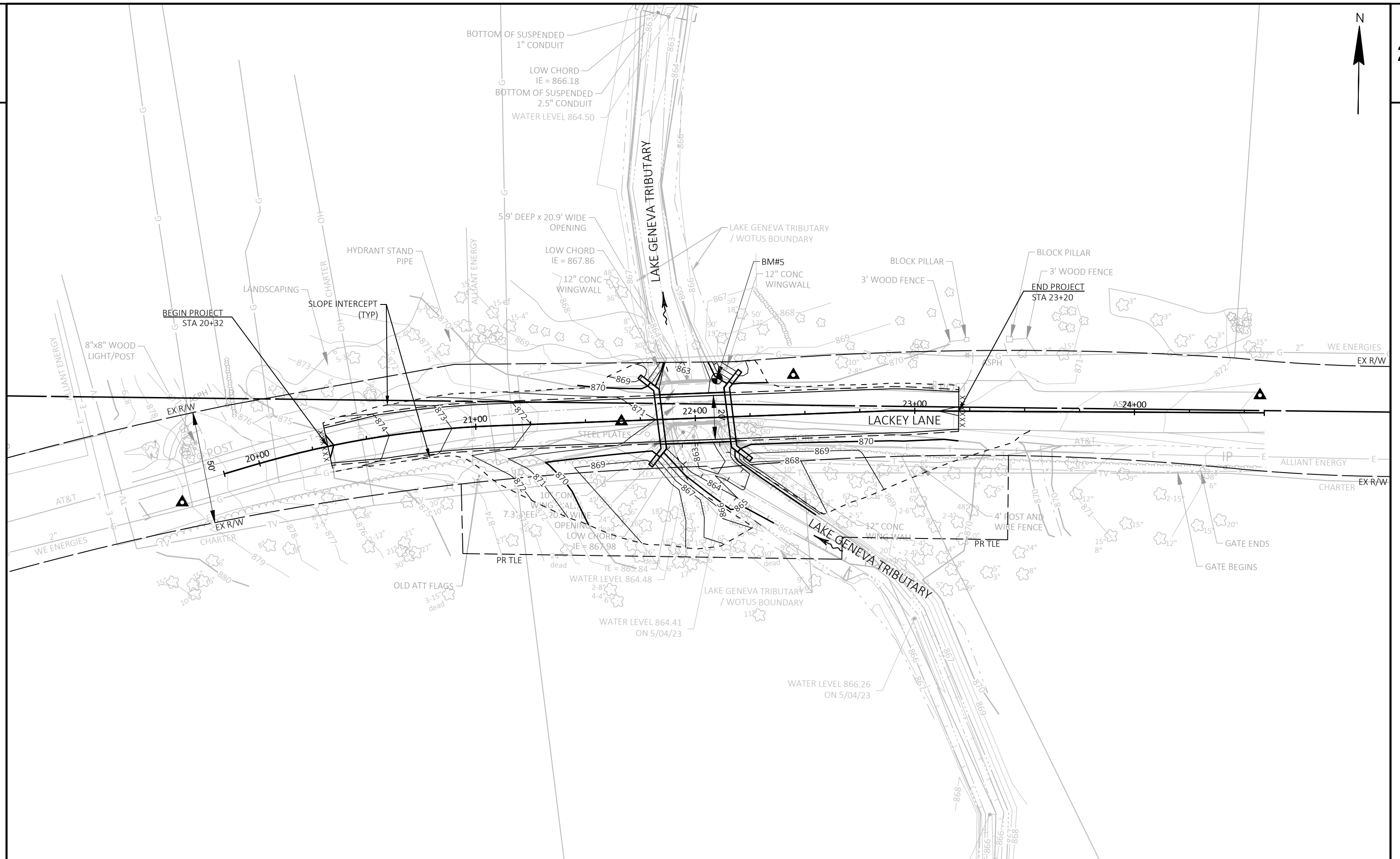
TEMPORARY SETTLING BASIN
(SIZE TO BE DETERMINED IN FIELD AS INDICATED ABOVE)



LEGEND

| | |
|---------------------------------------------------------------------------------------|-----------------------------------------------------------|
|  | TREE REMOVAL (375 ID) (BY OTHERS) |
|  | GRUBBING (310 SY) |
|  | REMOVING STRUCTURE OVER WATERWAY DEBRIS CAPTURE P-64-0906 |
|  | EXCAVATION COMMON |

| | | | | | |
|------------------------|------------------|------------------|--------------|-------|----------|
| PROJECT NO: 3846-00-75 | HWY: LACKEY LANE | COUNTY: WALWORTH | REMOVAL PLAN | SHEET | E |
|------------------------|------------------|------------------|--------------|-------|----------|



PROJECT NO: 3846-00-75

HWY: LACKEY LANE

COUNTY: WALWORTH

CONTOUR MAP

SHEET

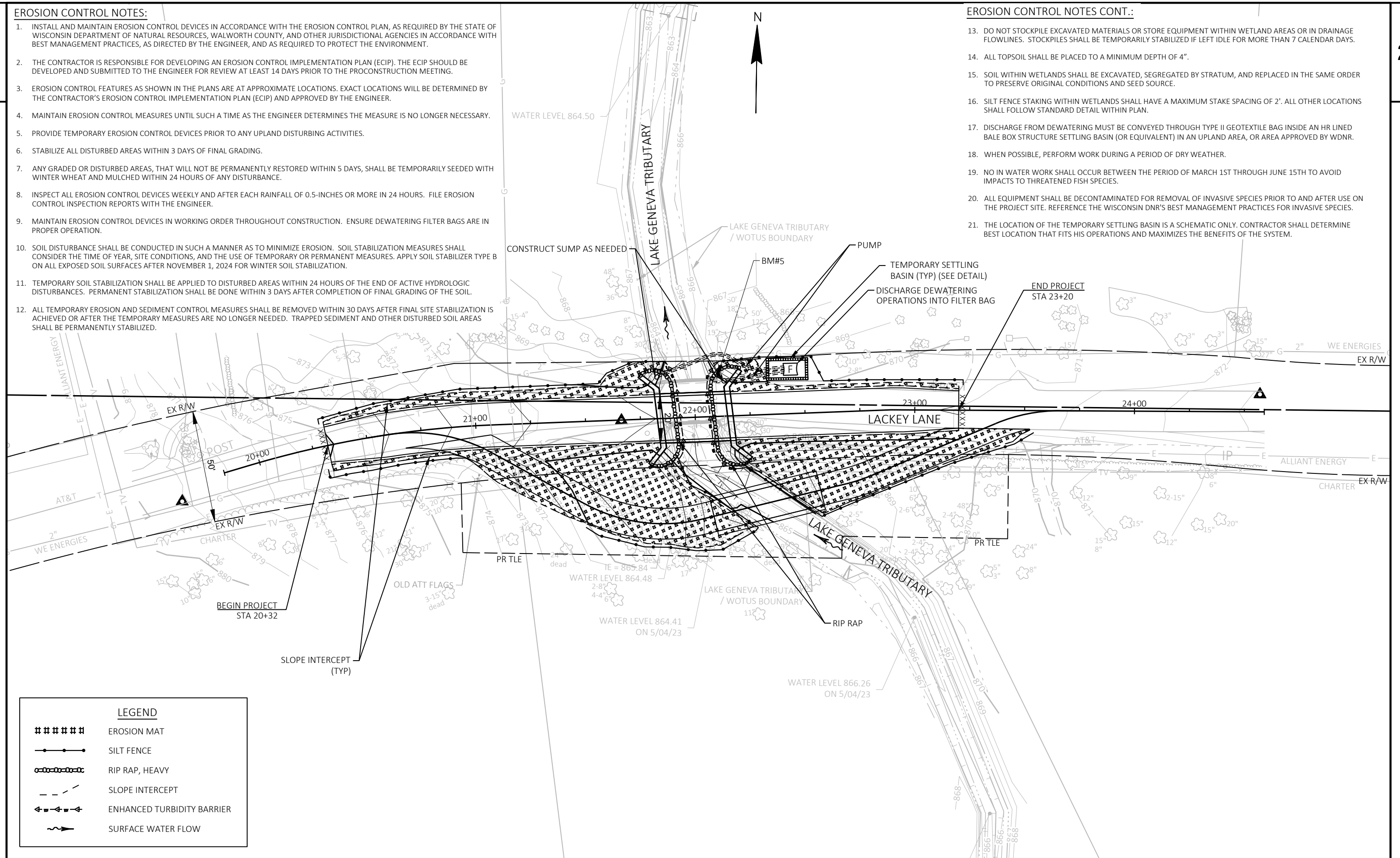
E

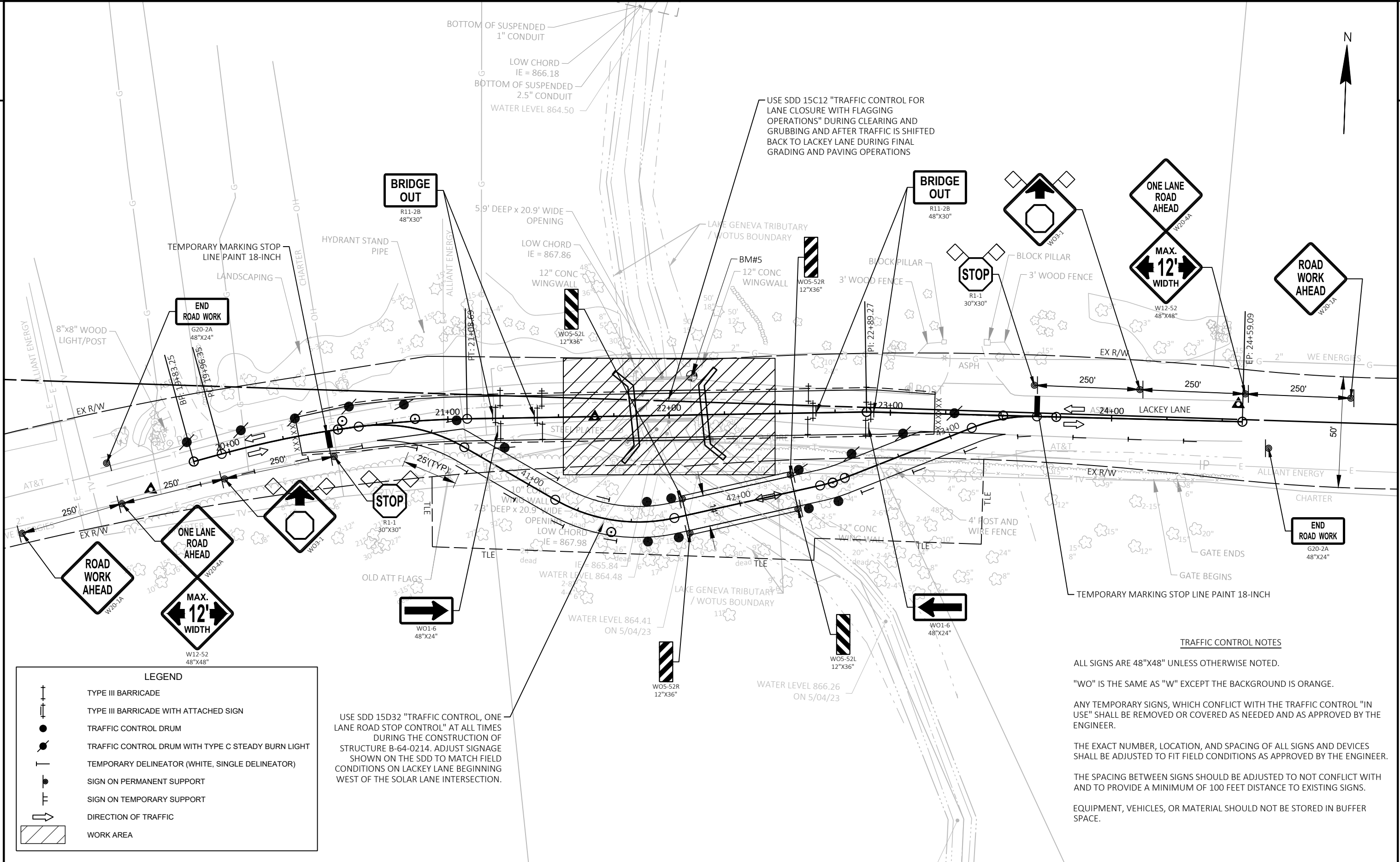
EROSION CONTROL NOTES:

1. INSTALL AND MAINTAIN EROSION CONTROL DEVICES IN ACCORDANCE WITH THE EROSION CONTROL PLAN, AS REQUIRED BY THE STATE OF WISCONSIN DEPARTMENT OF NATURAL RESOURCES, WALWORTH COUNTY, AND OTHER JURISDICTIONAL AGENCIES IN ACCORDANCE WITH BEST MANAGEMENT PRACTICES, AS DIRECTED BY THE ENGINEER, AND AS REQUIRED TO PROTECT THE ENVIRONMENT.
2. THE CONTRACTOR IS RESPONSIBLE FOR DEVELOPING AN EROSION CONTROL IMPLEMENTATION PLAN (ECIP). THE ECIP SHOULD BE DEVELOPED AND SUBMITTED TO THE ENGINEER FOR REVIEW AT LEAST 14 DAYS PRIOR TO THE PROCONSTRUCTION MEETING.
3. EROSION CONTROL FEATURES AS SHOWN IN THE PLANS ARE AT APPROXIMATE LOCATIONS. EXACT LOCATIONS WILL BE DETERMINED BY THE CONTRACTOR'S EROSION CONTROL IMPLEMENTATION PLAN (ECIP) AND APPROVED BY THE ENGINEER.
4. MAINTAIN EROSION CONTROL MEASURES UNTIL SUCH A TIME AS THE ENGINEER DETERMINES THE MEASURE IS NO LONGER NECESSARY.
5. PROVIDE TEMPORARY EROSION CONTROL DEVICES PRIOR TO ANY UPLAND DISTURBING ACTIVITIES.
6. STABILIZE ALL DISTURBED AREAS WITHIN 3 DAYS OF FINAL GRADING.
7. ANY GRADED OR DISTURBED AREAS, THAT WILL NOT BE PERMANENTLY RESTORED WITHIN 5 DAYS, SHALL BE TEMPORARILY SEEDED WITH WINTER WHEAT AND MULCHED WITHIN 24 HOURS OF ANY DISTURBANCE.
8. INSPECT ALL EROSION CONTROL DEVICES WEEKLY AND AFTER EACH RAINFALL OF 0.5-INCHES OR MORE IN 24 HOURS. FILE EROSION CONTROL INSPECTION REPORTS WITH THE ENGINEER.
9. MAINTAIN EROSION CONTROL DEVICES IN WORKING ORDER THROUGHOUT CONSTRUCTION. ENSURE DEWATERING FILTER BAGS ARE IN PROPER OPERATION.
10. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. SOIL STABILIZATION MEASURES SHALL CONSIDER THE TIME OF YEAR, SITE CONDITIONS, AND THE USE OF TEMPORARY OR PERMANENT MEASURES. APPLY SOIL STABILIZER TYPE B ON ALL EXPOSED SOIL SURFACES AFTER NOVEMBER 1, 2024 FOR WINTER SOIL STABILIZATION.
11. TEMPORARY SOIL STABILIZATION SHALL BE APPLIED TO DISTURBED AREAS WITHIN 24 HOURS OF THE END OF ACTIVE HYDROLOGIC DISTURBANCES. PERMANENT STABILIZATION SHALL BE DONE WITHIN 3 DAYS AFTER COMPLETION OF FINAL GRADING OF THE SOIL.
12. ALL TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED. TRAPPED SEDIMENT AND OTHER DISTURBED SOIL AREAS SHALL BE PERMANENTLY STABILIZED.

EROSION CONTROL NOTES CONT.:

13. DO NOT STOCKPILE EXCAVATED MATERIALS OR STORE EQUIPMENT WITHIN WETLAND AREAS OR IN DRAINAGE FLOWLINES. STOCKPILES SHALL BE TEMPORARILY STABILIZED IF LEFT IDLE FOR MORE THAN 7 CALENDAR DAYS.
14. ALL TOPSOIL SHALL BE PLACED TO A MINIMUM DEPTH OF 4".
15. SOIL WITHIN WETLANDS SHALL BE EXCAVATED, SEGREGATED BY STRATUM, AND REPLACED IN THE SAME ORDER TO PRESERVE ORIGINAL CONDITIONS AND SEED SOURCE.
16. SILT FENCE STAKING WITHIN WETLANDS SHALL HAVE A MAXIMUM STAKE SPACING OF 2'. ALL OTHER LOCATIONS SHALL FOLLOW STANDARD DETAIL WITHIN PLAN.
17. DISCHARGE FROM DEWATERING MUST BE CONVEYED THROUGH TYPE II GEOTEXTILE BAG INSIDE AN HR LINED BALE BOX STRUCTURE SETTLING BASIN (OR EQUIVALENT) IN AN UPLAND AREA, OR AREA APPROVED BY WDNR.
18. WHEN POSSIBLE, PERFORM WORK DURING A PERIOD OF DRY WEATHER.
19. NO IN WATER WORK SHALL OCCUR BETWEEN THE PERIOD OF MARCH 1ST THROUGH JUNE 15TH TO AVOID IMPACTS TO THREATENED FISH SPECIES.
20. ALL EQUIPMENT SHALL BE DECONTAMINATED FOR REMOVAL OF INVASIVE SPECIES PRIOR TO AND AFTER USE ON THE PROJECT SITE. REFERENCE THE WISCONSIN DNR'S BEST MANAGEMENT PRACTICES FOR INVASIVE SPECIES.
21. THE LOCATION OF THE TEMPORARY SETTLING BASIN IS A SCHEMATIC ONLY. CONTRACTOR SHALL DETERMINE BEST LOCATION THAT FITS HIS OPERATIONS AND MAXIMIZES THE BENEFITS OF THE SYSTEM.





USE SDD 15C12 "TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATIONS" DURING CLEARING AND GRUBBING AND AFTER TRAFFIC IS SHIFTED BACK TO LACKEY LANE DURING FINAL GRADING AND PAVING OPERATIONS

USE SDD 15D32 "TRAFFIC CONTROL, ONE LANE ROAD STOP CONTROL" AT ALL TIMES DURING THE CONSTRUCTION OF STRUCTURE B-64-0214. ADJUST SIGNAGE SHOWN ON THE SDD TO MATCH FIELD CONDITIONS ON LACKEY LANE BEGINNING WEST OF THE SOLAR LANE INTERSECTION.

LEGEND

- TYPE III BARRICADE
- TYPE III BARRICADE WITH ATTACHED SIGN
- TRAFFIC CONTROL DRUM
- TRAFFIC CONTROL DRUM WITH TYPE C STEADY BURN LIGHT
- TEMPORARY DELINEATOR (WHITE, SINGLE DELINEATOR)
- SIGN ON PERMANENT SUPPORT
- SIGN ON TEMPORARY SUPPORT
- DIRECTION OF TRAFFIC
- WORK AREA

TRAFFIC CONTROL NOTES

ALL SIGNS ARE 48"x48" UNLESS OTHERWISE NOTED.

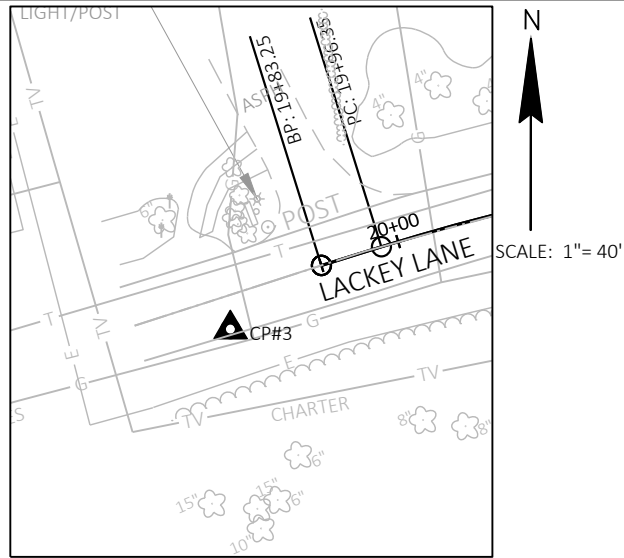
"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

ANY TEMPORARY SIGNS, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

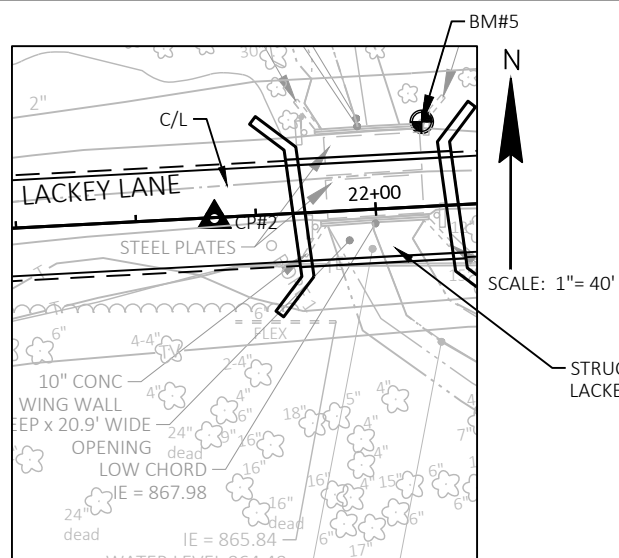
THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 100 FEET DISTANCE TO EXISTING SIGNS.

EQUIPMENT, VEHICLES, OR MATERIAL SHOULD NOT BE STORED IN BUFFER SPACE.



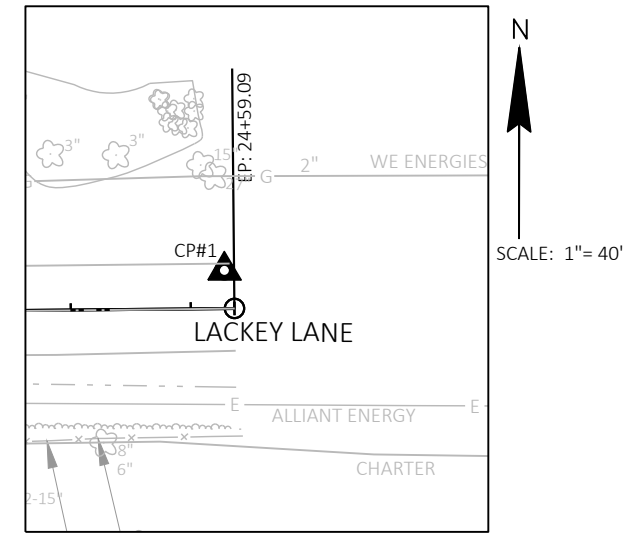
CONTROL POINT LOCATION

CP3 - PK NAIL IN ASPHALT
 Y = 324560.805
 X = 777298.671
 ELEV = 879.28



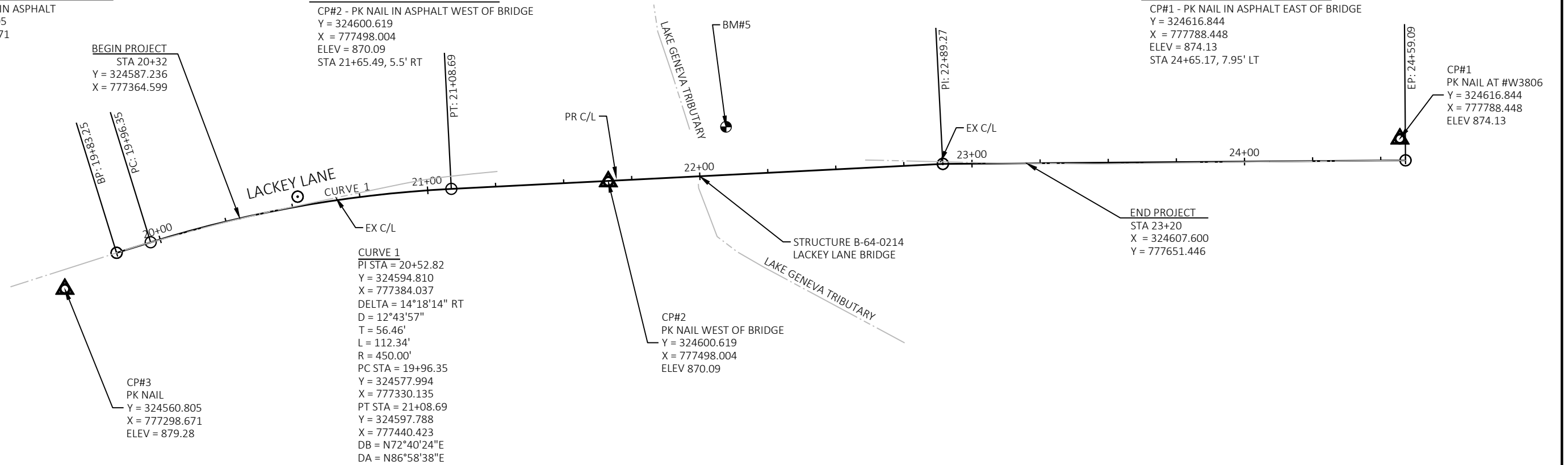
CONTROL POINT LOCATION

CP#2 - PK NAIL IN ASPHALT WEST OF BRIDGE
 Y = 324600.619
 X = 777498.004
 ELEV = 870.09
 STA 21+65.49, 5.5' RT



CONTROL POINT LOCATION

CP#1 - PK NAIL IN ASPHALT EAST OF BRIDGE
 Y = 324616.844
 X = 777788.448
 ELEV = 874.13
 STA 24+65.17, 7.95' LT



| BENCH MARK | | | | | |
|------------|-------------|-------------|------------------------|-----------|------------------------------------------------------------------|
| NO. | NORTHING | EASTING | STATION OFFSET | ELEVATION | DESCRIPTION |
| 5 | 324620.7540 | 777541.1890 | 22+09.29, 13.07' LT | 870.29 | X ON TOP OF NORTHEAST CONCRETE WINGWALL ON BRIDGE ON LACKEY LANE |

Estimate Of Quantities

3846-00-75

| Line | Item | Item Description | Unit | Total | Qty |
|------|------------|---------------------------------------------------------------------------|------|------------|------------|
| 0002 | 201.0210 | Grubbing | SY | 310.000 | 310.000 |
| 0004 | 203.0270 | Removing Structure Over Waterway Debris Capture (structure) 01. P-64-0906 | EACH | 1.000 | 1.000 |
| 0006 | 205.0100 | Excavation Common | CY | 172.000 | 172.000 |
| 0008 | 206.1001 | Excavation for Structures Bridges (structure) 01. B-64-0214 | EACH | 1.000 | 1.000 |
| 0010 | 210.1500 | Backfill Structure Type A | TON | 230.000 | 230.000 |
| 0012 | 213.0100 | Finishing Roadway (project) 01. 3846-00-75 | EACH | 1.000 | 1.000 |
| 0014 | 305.0110 | Base Aggregate Dense 3/4-Inch | TON | 14.000 | 14.000 |
| 0016 | 305.0120 | Base Aggregate Dense 1 1/4-Inch | TON | 424.000 | 424.000 |
| 0018 | 305.0410 | Aggregate Detours | TON | 143.000 | 143.000 |
| 0020 | 455.0605 | Tack Coat | GAL | 39.000 | 39.000 |
| 0022 | 460.2000 | Incentive Density HMA Pavement | DOL | 90.000 | 90.000 |
| 0024 | 460.5223 | HMA Pavement 3 LT 58-28 S | TON | 72.000 | 72.000 |
| 0026 | 460.5224 | HMA Pavement 4 LT 58-28 S | TON | 56.000 | 56.000 |
| 0028 | 502.0100 | Concrete Masonry Bridges | CY | 97.000 | 97.000 |
| 0030 | 502.3200 | Protective Surface Treatment | SY | 105.000 | 105.000 |
| 0032 | 502.3210 | Pigmented Surface Sealer | SY | 34.000 | 34.000 |
| 0034 | 505.0400 | Bar Steel Reinforcement HS Structures | LB | 3,480.000 | 3,480.000 |
| 0036 | 505.0600 | Bar Steel Reinforcement HS Coated Structures | LB | 19,320.000 | 19,320.000 |
| 0038 | 516.0500 | Rubberized Membrane Waterproofing | SY | 10.000 | 10.000 |
| 0040 | 517.1010.S | Concrete Staining (structure) 01. B-64-0214 | SF | 49.000 | 49.000 |
| 0042 | 517.1015.S | Concrete Staining Multi-Color (structure) 01. B-64-0214 | SF | 135.000 | 135.000 |
| 0044 | 517.1050.S | Architectural Surface Treatment (structure) 01. B-64-0214 | SF | 135.000 | 135.000 |
| 0046 | 526.0101 | Temporary Structure (station) 01. STA 42+00 | EACH | 1.000 | 1.000 |
| 0048 | 550.2104 | Piling CIP Concrete 10 3/4 X 0.25-Inch | LF | 1,040.000 | 1,040.000 |
| 0050 | 606.0300 | Riprap Heavy | CY | 72.000 | 72.000 |
| 0052 | 612.0406 | Pipe Underdrain Wrapped 6-Inch | LF | 130.000 | 130.000 |
| 0054 | 618.0100 | Maintenance and Repair of Haul Roads (project) 01. 3846-00-75 | EACH | 1.000 | 1.000 |
| 0056 | 619.1000 | Mobilization | EACH | 1.000 | 1.000 |
| 0058 | 624.0100 | Water | MGAL | 8.900 | 8.900 |
| 0060 | 625.0100 | Topsoil | SY | 1,012.000 | 1,012.000 |
| 0062 | 627.0200 | Mulching | SY | 1,012.000 | 1,012.000 |
| 0064 | 628.1504 | Silt Fence | LF | 620.000 | 620.000 |
| 0066 | 628.1520 | Silt Fence Maintenance | LF | 620.000 | 620.000 |
| 0068 | 628.1905 | Mobilizations Erosion Control | EACH | 2.000 | 2.000 |
| 0070 | 628.1910 | Mobilizations Emergency Erosion Control | EACH | 2.000 | 2.000 |
| 0072 | 628.2008 | Erosion Mat Urban Class I Type B | SY | 1,134.000 | 1,134.000 |
| 0074 | 628.6510 | Soil Stabilizer Type B | ACRE | 0.380 | 0.380 |
| 0076 | 629.0210 | Fertilizer Type B | CWT | 0.650 | 0.650 |
| 0078 | 630.0130 | Seeding Mixture No. 30 | LB | 18.300 | 18.300 |
| 0080 | 630.0200 | Seeding Temporary | LB | 58.300 | 58.300 |
| 0082 | 630.0500 | Seed Water | MGAL | 18.200 | 18.200 |
| 0084 | 633.1100 | Delineators Temporary | EACH | 22.000 | 22.000 |
| 0086 | 634.0612 | Posts Wood 4x6-Inch X 12-FT | EACH | 4.000 | 4.000 |
| 0088 | 637.2230 | Signs Type II Reflective F | SF | 12.000 | 12.000 |
| 0090 | 643.0300 | Traffic Control Drums | DAY | 1,058.000 | 1,058.000 |
| 0092 | 643.0420 | Traffic Control Barricades Type III | DAY | 644.000 | 644.000 |
| 0094 | 643.0705 | Traffic Control Warning Lights Type A | DAY | 598.000 | 598.000 |
| 0096 | 643.0900 | Traffic Control Signs | DAY | 1,548.000 | 1,548.000 |
| 0098 | 643.3805 | Temporary Marking Stop Line Paint 18-Inch | LF | 18.000 | 18.000 |
| 0100 | 643.5000 | Traffic Control | EACH | 1.000 | 1.000 |

Estimate Of Quantities

3846-00-75

| Line | Item | Item Description | Unit | Total | Qty |
|------|----------|--------------------------------------------------------------------|------|---------|---------|
| 0102 | 645.0111 | Geotextile Type DF Schedule A | SY | 78.000 | 78.000 |
| 0104 | 645.0120 | Geotextile Type HR | SY | 152.000 | 152.000 |
| 0106 | 645.0220 | Geogrid Type SR | SY | 473.000 | 473.000 |
| 0108 | 650.5000 | Construction Staking Base | LF | 252.000 | 252.000 |
| 0110 | 650.6501 | Construction Staking Structure Layout (structure) 01. B-64-0214 | EACH | 1.000 | 1.000 |
| 0112 | 650.9911 | Construction Staking Supplemental Control (project) 01. 3846-00-75 | EACH | 1.000 | 1.000 |
| 0114 | 690.0150 | Sawing Asphalt | LF | 33.000 | 33.000 |
| 0116 | 715.0502 | Incentive Strength Concrete Structures | DOL | 582.000 | 582.000 |
| 0118 | ASP.1T0A | On-the-Job Training Apprentice at \$5.00/HR | HRS | 350.000 | 350.000 |
| 0120 | ASP.1T0G | On-the-Job Training Graduate at \$5.00/HR | HRS | 900.000 | 900.000 |
| 0122 | SPV.0180 | Special 01. Enhanced Turbidity Barrier | SY | 54.000 | 54.000 |

| DIVISION | FROM/TO STATION | LOCATION | 205.0100 COMMON EXCAVATION (1) | | SALVAGED/UNUSABLE PAVEMENT MATERIAL (4) | AVAILABLE MATERIAL (5) | UNEXPANDED FILL | EXPANDED FILL (13) | MASS ORDINATE +/- (14) | WASTE | 208.0100 BORROW | COMMENT |
|---------------------------------|-----------------------|-------------|--------------------------------|--------------------|-----------------------------------------|------------------------|-----------------|--------------------|------------------------|-------|-----------------|---------|
| | | | CUT (2) | EBS EXCAVATION (3) | | | | FACTOR 1.10 | | | | |
| DIVISION 1 | | | | | | | | | | | | |
| Lackey Lane - West of Structure | STA 20+32 - STA 21+82 | Lackey Lane | 73 | 0 | 34 | 39 | 28 | 31 | 8 | | | |
| Lackey Lane - East of Structure | STA 22+18 - STA 23+20 | Lackey Lane | 83 | 0 | 22 | 61 | 15 | 17 | 44 | | | |
| DIVISION 1 SUBTOTAL | | | 156 | 0 | 56 | 100 | 43 | 47 | 52 | 52 | 0 | |
| DIVISION 2 | | | | | | | | | | | | |
| Bypass Road - West of Structure | STA 40+25 - STA 41+75 | Bypass Road | 12 | 0 | 0 | 12 | 32 | 35 | -23 | | | |
| Bypass Road - East of Structure | STA 42+25 - STA 43+25 | Bypass Road | 4 | 0 | 0 | 4 | 2 | 2 | 2 | | | |
| DIVISION 2 SUBTOTAL | | | 16 | 0 | 0 | 16 | 34 | 37 | -21 | 0 | 21 | |
| GRAND TOTAL | | | 172 | 0 | 56 | 116 | 77 | 85 | 31 | 52 | 21 | |
| TOTAL COMMON EXC | | | 172 | | | | | | | 31 | | |

NOTES:

- (1) COMMON EXCAVATION IS THE SUM OF THE CUT AND EBS EXCAVATION COLUMNS. ITEM NUMBER 205.0100
- (2) SALVAGED/UNUSABLE PAVEMENT MATERIAL IS INCLUDED IN CUT.
- (3) EBS EXCAVATION TO BE BACKFILLED WITH SELECT BORROW MATERIAL.
- (4) SALVAGED/UNUSABLE PAVEMENT MATERIAL
- (5) AVAILABLE MATERIAL = CUT - SALVAGED/UNUSABLE PAVEMENT MATERIAL
- (13) EXPANDED FILL FACTOR = 1.1 EXPANDED FILL = (UNEXPANDED FILL - EXPANDED ROCK) * FILL FACTOR
- (14) THE MASS ORDINATE + OR - QTY CALCULATED FOR THE DIVISION. PLUS QUANTITY INDICATES AN EXCESS OF MATERIAL WITHIN THE DIVISION. MINUS INDICATES A SHORTAGE OF MATERIAL WITHIN THE DIVISION.

GRUBBING

| CATEGORY | STATION TO STATION | LOCATION | 201.0210 GRUBBING SY | REMARKS |
|------------|--------------------|--------------------|----------------------|---------|
| 0010 | 20+32 - 23+20 | PROJECT 3846-00-75 | 310 | |
| TOTAL 0010 | | | 310 | |

FINISHING ROADWAY

| CATEGORY | STATION TO STATION | LOCATION | 213.0100.01 FINISHING ROADWAY (PROJECT) (01. 3846-00-75) EACH | REMARKS |
|------------|--------------------|--------------------|---------------------------------------------------------------|---------|
| 0010 | 20+32 - 23+20 | PROJECT 3846-00-75 | 1 | |
| TOTAL 0010 | | | 1 | |

3

EROSION CONTROL

| CATEGORY | STATION TO STATION | LOCATION | 628.1504 SILT FENCE LF | 628.1520 SILT FENCE MAINTENANCE LF | 628.1905 MOBILIZATIONS EROSION CONTROL EACH | 628.1910 MOBILIZATIONS EMERGENCY EROSION CONTROL EACH | SPV.0180.01 SPECIAL (01. ENHANCED TURBIDITY BARRIER) SY | REMARKS |
|------------|--------------------|--------------------|------------------------------|---------------------------------------------|---------------------------------------------------------|----------------------------------------------------------------------|------------------------------------------------------------------------|---------|
| 0010 | 20+32 - 21+83 | LACKEY LANE | 222 | 222 | - | - | - | |
| 0010 | 22+17 - 23+20 | LACKEY LANE | 120 | 120 | - | - | - | |
| 0010 | 40+25 - 41+75 | TEMPORARY BYPASS | 133 | 133 | - | - | - | |
| 0010 | 42+25 - 43+25 | TEMPORARY BYPASS | 145 | 145 | - | - | - | |
| 0010 | 20+32 - 23+20 | PROJECT 3846-00-75 | - | - | 2 | 2 | - | |
| TOTAL 0010 | | | 620 | 620 | 2 | 2 | - | |
| 0020 | 20+32 - 21+83 | LACKEY LANE | - | - | - | - | 27 | |
| 0020 | 22+17 - 23+20 | LACKEY LANE | - | - | - | - | 27 | |
| | | UNDISTRIBUTED | - | - | - | - | - | |
| TOTAL 0020 | | | - | - | - | - | 54 | |

HAUL ROAD MAINTENANCE & REPAIR

| CATEGORY | STATION TO STATION | LOCATION | 618.0100.01 MAINTENANCE AND REPAIR OF HAUL ROADS (PROJECT) (01. 3846-00-75) EACH | REMARKS |
|------------|--------------------|--------------------|----------------------------------------------------------------------------------------------|---------|
| 0030 | 20+32 - 23+20 | PROJECT 3846-00-75 | 1 | |
| TOTAL 0030 | | | 1 | |

3

BASE AGGREGATE

| CATEGORY | STATION TO STATION | LOCATION | 305.0110 BASE AGGREGATE DENSE 3/4-INCH TON | 305.0120 BASE AGGREGATE DENSE 1 1/4-INCH TON | 305.0410 AGGREGATE DETOURS TON | 624.0100 WATER MGAL | 645.0220 GEOGRID TYPE SR SY | REMARKS |
|------------|--------------------|------------------|-----------------------------------------------------|-------------------------------------------------------|-----------------------------------------|---------------------------|--------------------------------------|----------------------------------|
| 0010 | 20+32 - 21+83 | LACKEY LANE | 8 | 240 | - | 5.0 | - | |
| 0010 | 22+17 - 23+20 | LACKEY LANE | 6 | 163 | - | 3.4 | - | |
| 0010 | | UNDISTRIBUTED | - | 21 | - | 0.5 | - | WEDGE TO STRUCTURE (OVER WINTER) |
| TOTAL 0010 | | | 14 | 424 | - | 8.9 | - | |
| 0020 | 40+25 - 41+75 | TEMPORARY BYPASS | - | - | 86 | - | 284 | |
| 0020 | 42+25 - 43+25 | TEMPORARY BYPASS | - | - | 57 | - | 189 | |
| TOTAL 0020 | | | - | - | 143 | - | 473 | |

SAWING

| CATEGORY | STATION TO STATION | LOCATION | 690.0150 SAWING ASPHALT LF | REMARKS |
|------------|--------------------|-------------|-------------------------------------|---------|
| 0010 | 20+32 - 21+83 | LACKEY LANE | 16 | |
| 0010 | 22+17 - 23+20 | LACKEY LANE | 17 | |
| TOTAL 0010 | | | 33 | |

CONSTRUCTION STAKING

| CATEGORY | STATION TO STATION | LOCATION | 650.5000 CONSTRUCTION STAKING BASE LF | 650.6501.01 CONSTRUCTION STAKING STRUCTURE LAYOUT (STRUCTURE) (01. B-64-0214) EACH | 650.9911.01 CONSTRUCTION STAKING SUPPLEMENTAL CONTROL (PROJECT) (01. 3846-00-75) EACH | REMARKS |
|------------|--------------------|--------------------|------------------------------------------------|------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------|---------|
| 0010 | 20+32 - 23+20 | PROJECT 3846-00-75 | 252 | - | - | |
| TOTAL 0010 | | | 252 | - | - | |
| 0020 | 20+32 - 23+20 | PROJECT 3846-00-75 | - | 1 | 1 | |
| TOTAL 0020 | | | - | 1 | 1 | |

HMA PAVEMENT

| CATEGORY | STATION TO STATION | LOCATION | 455.0605 TACK COAT GAL | 460.5223 HMA PAVEMENT 3 LT 58-28 S TON | 460.5224 HMA PAVEMENT 4 LT 58-28 S TON | REMARKS |
|------------|--------------------|-------------|------------------------------|-------------------------------------------------|-------------------------------------------------|---------|
| 0010 | 20+32 - 23+20 | LACKEY LANE | 39.0 | 72 | 56 | |
| TOTAL 0010 | | | 39 | 72 | 56 | |

PROJECT NO: 3846-00-75

HWY: LACKEY LANE

COUNTY: WALWORTH

MISCELLANEOUS QUANTITIES

SHEET

E

TRAFFIC CONTROL

| CATEGORY | STATION TO STATION | LOCATION | DAYS | 633.1100 | 643.0300 | 643.0420 | 643.0705 | 643.0900 | 643.3805 | 643.5000 | REMARKS |
|------------|--------------------|--------------------|------|----------------------------------|------------------------------------|-----------------------------------------------------|-------------------------------------------------------|------------------------------------|----------------------------------------------------------|----------------------------|-------------------------------|
| | | | | DELINEATORS TEMPORARY EACH | TRAFFIC CONTROL DRUMS DAY | TRAFFIC CONTROL BARRICADES TYPE III DAY | TRAFFIC CONTROL WARNING LIGHTS TYPE A DAY | TRAFFIC CONTROL SIGNS DAY | TEMPORARY MARKING STOP LINE PAINT 18-INCH LF | TRAFFIC CONTROL EACH | |
| 0010 | 20+32 - 23+20 | FLAGGING OPERATION | 7 | - | - | - | - | 56 | - | - | TEMP BRIDGE/ROAD CONSTRUCTION |
| 0010 | 40+25 - 43+25 | TEMPORARY BYPASS | 46 | 18 | 782 | 552 | 506 | 1,196 | 18 | - | BRIDGE/ROAD CONSTRUCTION |
| 0010 | 20+32 - 23+20 | FLAGGING OPERATION | 7 | - | - | - | - | 56 | - | - | TEMP BRIDGE/ROAD REMOVAL |
| 0010 | 20+32 - 23+20 | FLAGGING OPERATION | 7 | - | - | - | - | 56 | - | - | HMA & GRAVEL SHOULDERS |
| 0010 | 20+32 - 23+20 | PROJECT 3846-00-75 | - | - | - | - | - | - | - | 1 | |
| 0010 | 20+32 - 23+20 | UNDISTRIBUTED | 46 | 4 | 276 | 92 | 92 | 184 | - | - | |
| TOTAL 0010 | | | | 22 | 1,058 | 644 | 598 | 1,548 | 18 | 1 | |

LAWN RESTORATION

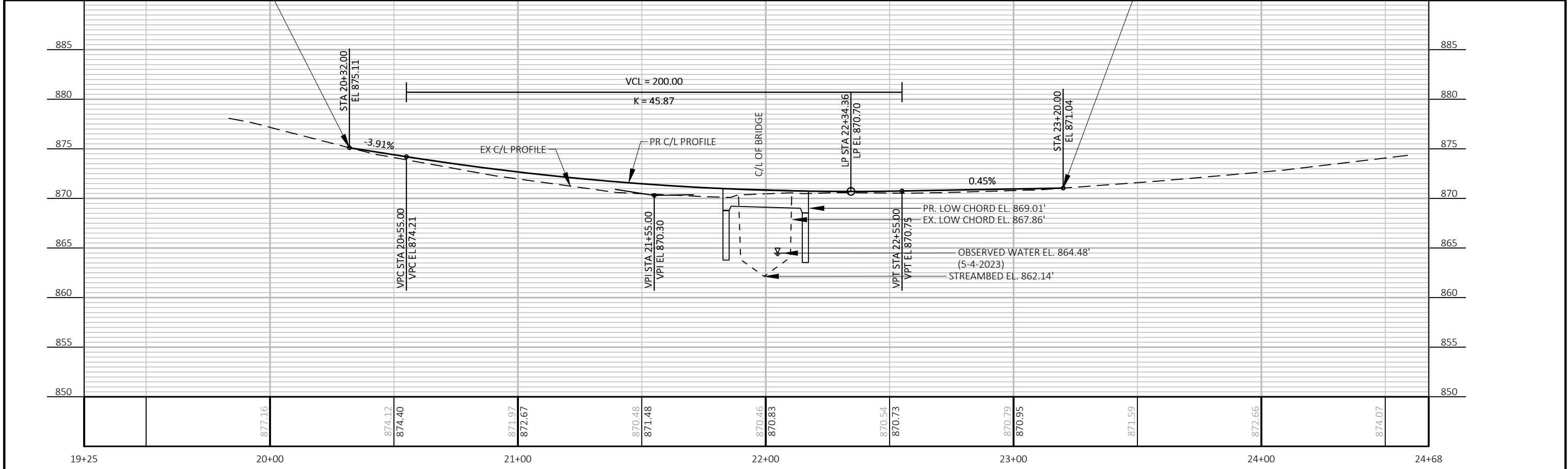
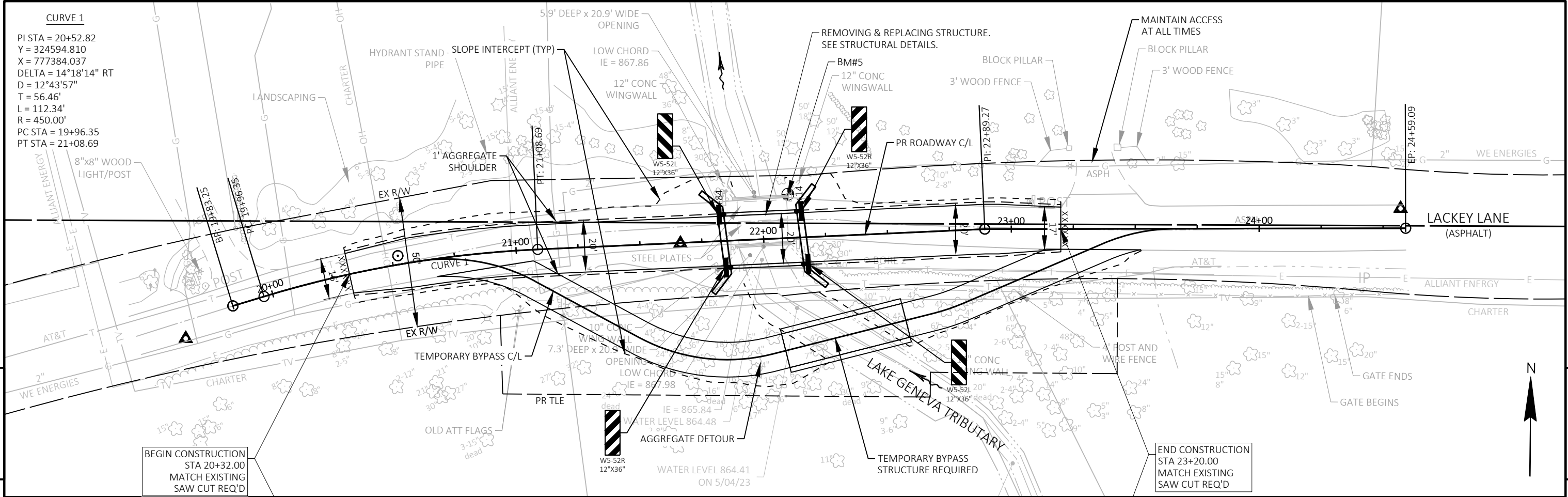
| CATEGORY | STATION TO STATION | LOCATION | 625.0100 | 627.0200 | 628.2008 | 628.6510 | 629.0210 | 630.0130 | 630.0200 | 630.0500 | REMARKS |
|------------|--------------------|------------------|---------------|----------------|----------------------------------------------|-----------------------------------|-----------------------------|---------------------------------|----------------------------|--------------------|--------------------------------|
| | | | TOPSOIL SY | MULCHING SY | EROSION MAT URBAN CLASS I TYPE B SY | SOIL STABILIZER TYPE B ACRE | FERTILIZER TYPE B CWT | SEEDING MIXTURE NO. 30 LB | SEEDING TEMPORARY LB | SEED WATER MGAL | |
| 0010 | 20+32 - 21+83 | LACKEY LANE | 489 | - | 489 | - | 0.31 | 8.8 | - | 2.8 | PERMANENT RESTORATION - SPRING |
| 0010 | 20+32 - 21+83 | LACKEY LANE | - | 489 | - | - | - | - | 13.2 | 2.8 | TEMPORARY RESTORATION |
| 0010 | 20+32 - 21+83 | LACKEY LANE | - | - | - | 0.20 | - | - | 13.2 | 2.8 | OVER WINTERING |
| 0010 | 22+17 - 23+20 | LACKEY LANE | 320 | - | 320 | - | 0.21 | 5.8 | - | 1.8 | PERMANENT RESTORATION - SPRING |
| 0010 | 22+17 - 23+20 | LACKEY LANE | - | 320 | - | - | - | - | 8.7 | 1.8 | TEMPORARY RESTORATION |
| 0010 | 22+17 - 23+20 | LACKEY LANE | - | - | - | 0.10 | - | - | 8.7 | 1.8 | OVER WINTERING |
| 0010 | 40+25 - 41+75 | TEMPORARY BYPASS | - | - | 75 | - | - | - | 2.1 | 0.5 | |
| 0010 | 42+25 - 43+25 | TEMPORARY BYPASS | - | - | 23 | - | - | - | 0.7 | 0.2 | |
| 0010 | | UNDISTRIBUTED | 203 | 203 | 227 | 0.08 | 0.13 | 3.7 | 11.7 | 3.7 | |
| TOTAL 0010 | | | 1,012 | 1,012 | 1,134 | 0.38 | 0.65 | 18.3 | 58.3 | 18.2 | |

MOBILIZATION

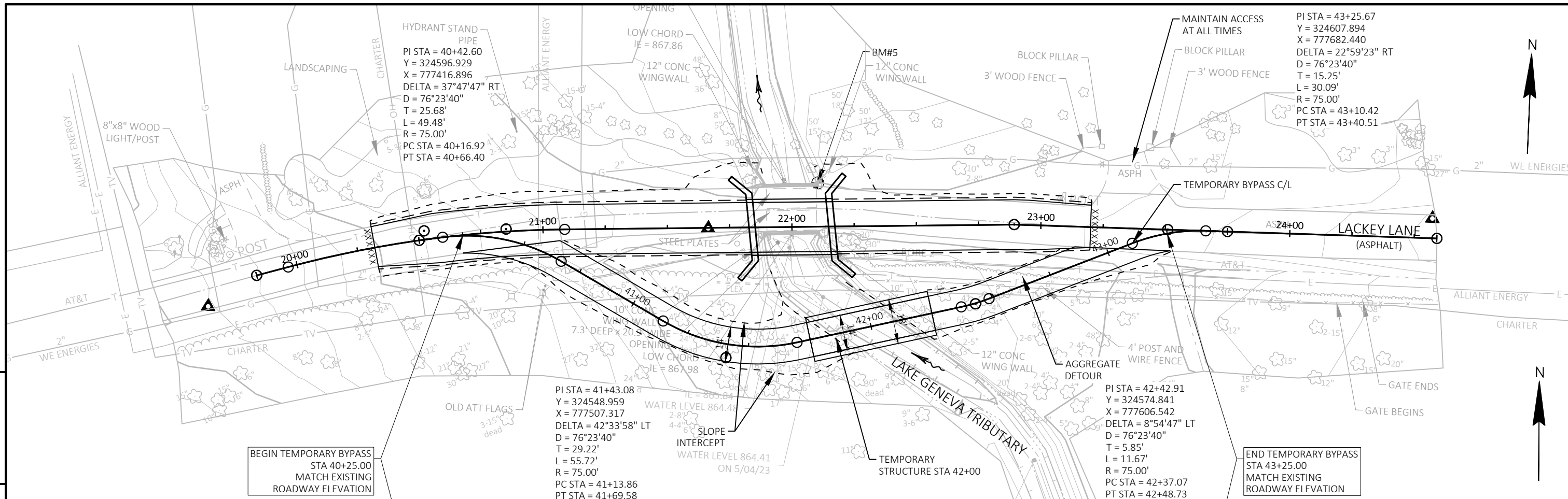
| CATEGORY | STATION TO STATION | LOCATION | 619.1000 MOBILIZATION EACH | REMARKS |
|------------|--------------------|--------------------|----------------------------------|---------|
| 0020 | 20+32 - 23+20 | PROJECT 3846-00-75 | 1 | |
| TOTAL 0020 | | | 1 | |

PERMANENT SIGNING

| CATEGORY | STATION SIDE | TYPE | SIZE | LOCATION | 634.0612 | 637.2230 | REMARKS |
|------------|--------------|------|----------------|-------------------------|-----------------------------------------|-------------------------------------|---------|
| | | | | | POSTS WOOD 4X6- INCH X 12-FT EACH | SIGNS TYPE II REFLECTIVE F SF | |
| 0020 | 21+84 | LT | W5-52L 12"X36" | LACKEY LANE - B-66-0214 | 1 | 3 | |
| 0020 | 21+86 | RT | W5-52R 12"X36" | LACKEY LANE - B-66-0214 | 1 | 3 | |
| 0020 | 22+14 | LT | W5-52R 12"X36" | LACKEY LANE - B-66-0214 | 1 | 3 | |
| 0020 | 22+16 | RT | W5-52L 12"X36" | LACKEY LANE - B-66-0214 | 1 | 3 | |
| TOTAL 0020 | | | | | 4 | 12 | |



| | | | | | | | | | |
|-------------|------------|------|-------------|---------|----------|-------------------|-------------|-------|---|
| PROJECT NO: | 3846-00-75 | HWY: | LACKEY LANE | COUNTY: | WALWORTH | PLAN AND PROFILE: | LACKEY LANE | SHEET | 5 |
|-------------|------------|------|-------------|---------|----------|-------------------|-------------|-------|---|



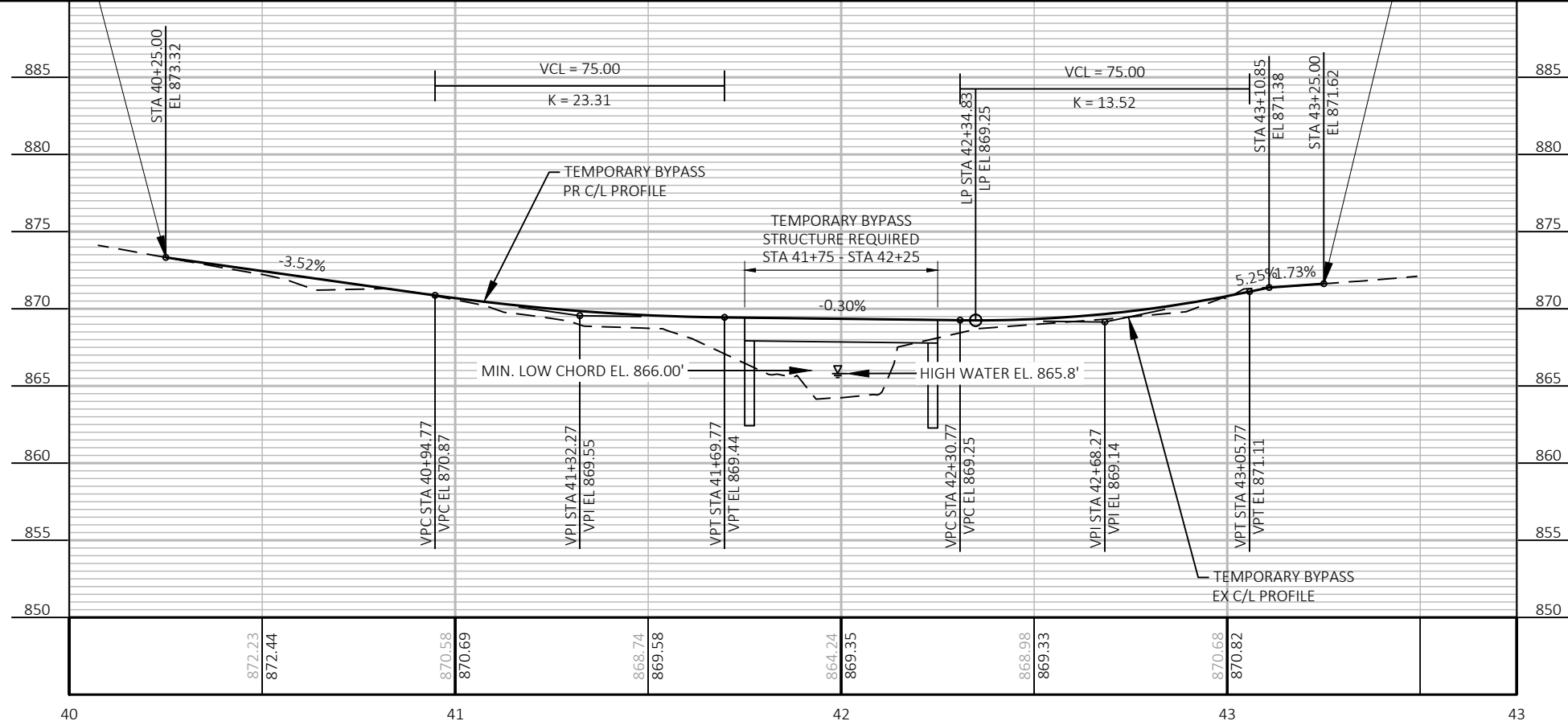
BEGIN TEMPORARY BYPASS
STA 40+25.00
MATCH EXISTING
ROADWAY ELEVATION

PI STA = 41+43.08
Y = 324548.959
X = 777507.317
DELTA = 42°33'58" LT
D = 76°23'40"
T = 29.22'
L = 55.72'
R = 75.00'
PC STA = 41+13.86
PT STA = 41+69.58

PI STA = 42+42.91
Y = 324574.841
X = 777606.542
DELTA = 8°54'47" LT
D = 76°23'40"
T = 5.85'
L = 11.67'
R = 75.00'
PC STA = 42+37.07
PT STA = 42+48.73

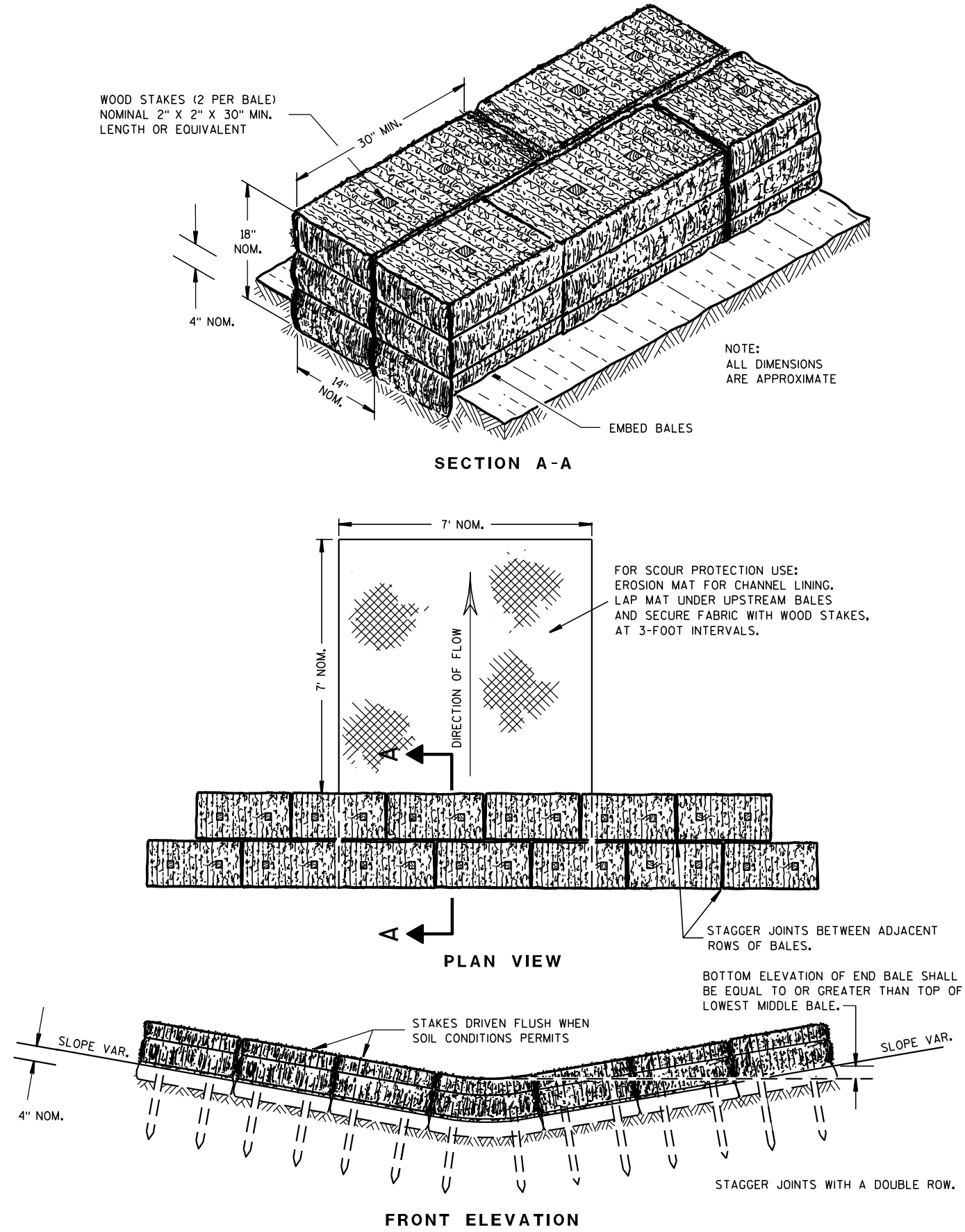
END TEMPORARY BYPASS
STA 43+25.00
MATCH EXISTING
ROADWAY ELEVATION

TEMPORARY BYPASS STRUCTURE
STA 41+75 - STA 42+25
HIGH WATER ELEV. = 865.8'
Q5 = 100 CFS
MIN. CLEAR OPENING AREA OF TEMP. STRUCTURE = 21.5 SF
MIN. 50' CLEAR SPAN
MIN. LOW CHORD ELEV. = 866.0'
14' CLEAR WIDTH BETWEEN RAILS



Standard Detail Drawing List

| | |
|-----------|-------------------------------------------------------------------|
| 08E08-03 | TYPICAL INSTALLATIONS OF EROSION BALES / TEMPORARY DITCH CHECKS |
| 08E09-06 | SILT FENCE |
| 08E11-02 | TURBIDITY BARRIER |
| 12A03-10 | NAME PLATE (STRUCTURES) |
| 15A04-07A | FLEXIBLE DELINEATOR POST |
| 15C11-10B | CHANNELIZING DEVICES DRUMS, CONES, BARRICADES AND VERTICAL PANELS |
| 15C12-09A | TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION |
| 15D31-05 | TRAFFIC CONTROL, TEMPORARY BYPASS ROADWAY |
| 15D32-07 | TRAFFIC CONTROL, ONE LANE ROAD STOP CONDITION |

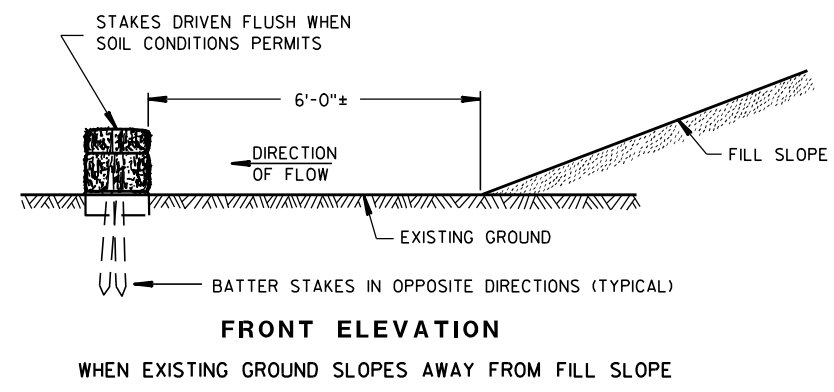
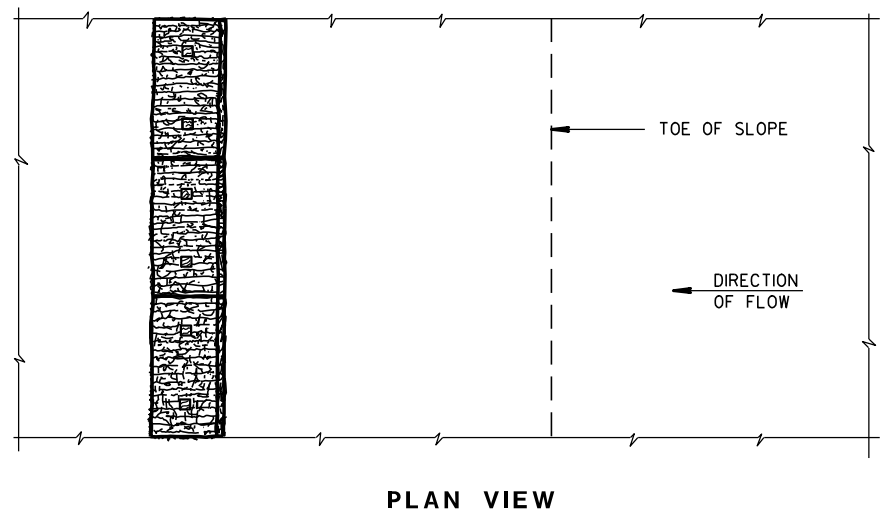
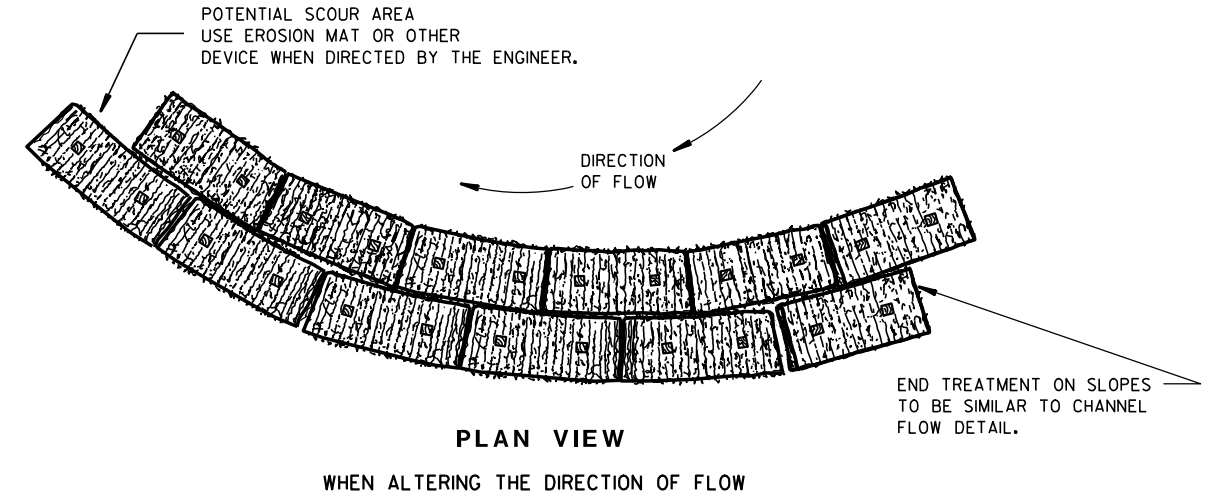


TEMPORARY DITCH CHECK USING EROSION BALES ①

GENERAL NOTES

DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

- ① TEMPORARY DITCH CHECKS EITHER EROSION BALES OR MANUFACTURED SHALL BE PAID FOR UNDER THE BID ITEM OF TEMPORARY DITCH CHECK. THE DEPARTMENT WILL NOT PAY FOR TEMPORARY DITCH CHECKS CONSTRUCTED OF A SINGLE ROW OF EROSION BALES.

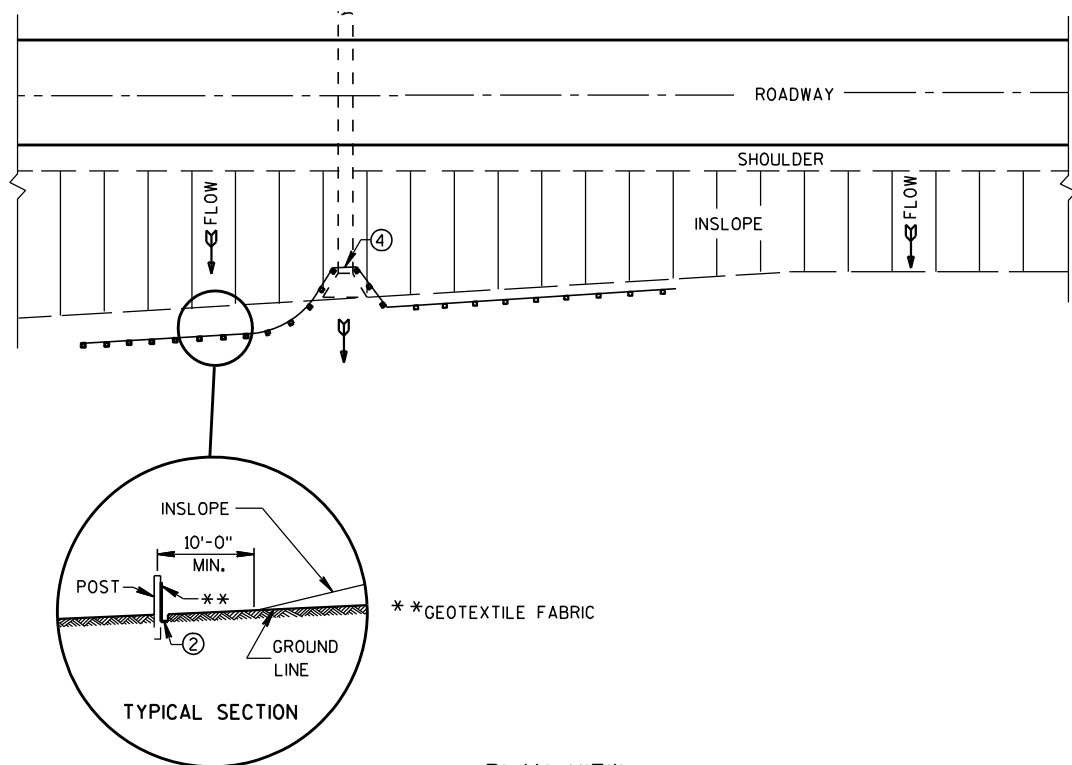


EROSION BALES FOR SHEET FLOW

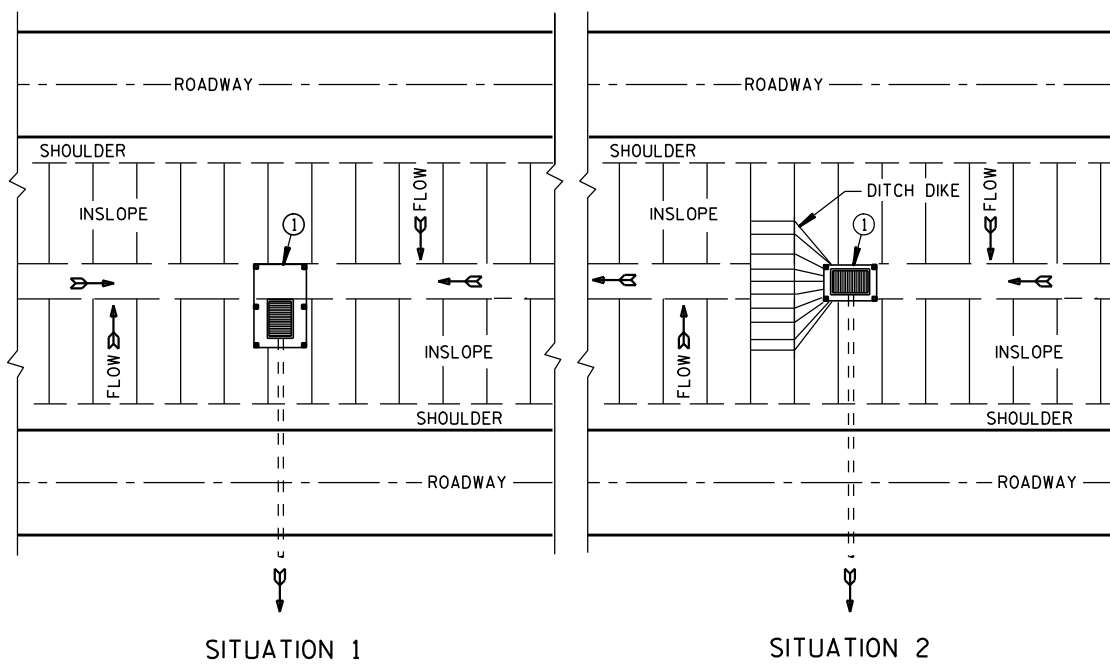
TYPICAL INSTALLATIONS OF EROSION BALES / TEMPORARY DITCH CHECKS

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
6/04/02 /S/ Beth Canestra
DATE CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA



PLAN VIEW
TYPICAL APPLICATION OF SILT FENCE

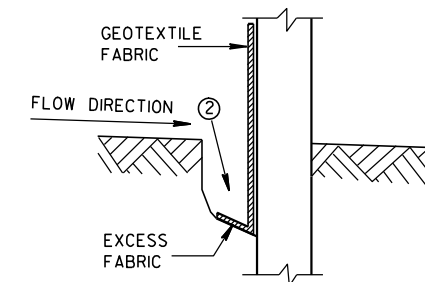


SITUATION 1 SITUATION 2
PLAN VIEW
SILT FENCE AT MEDIAN SURFACE DRAINS

GENERAL NOTES

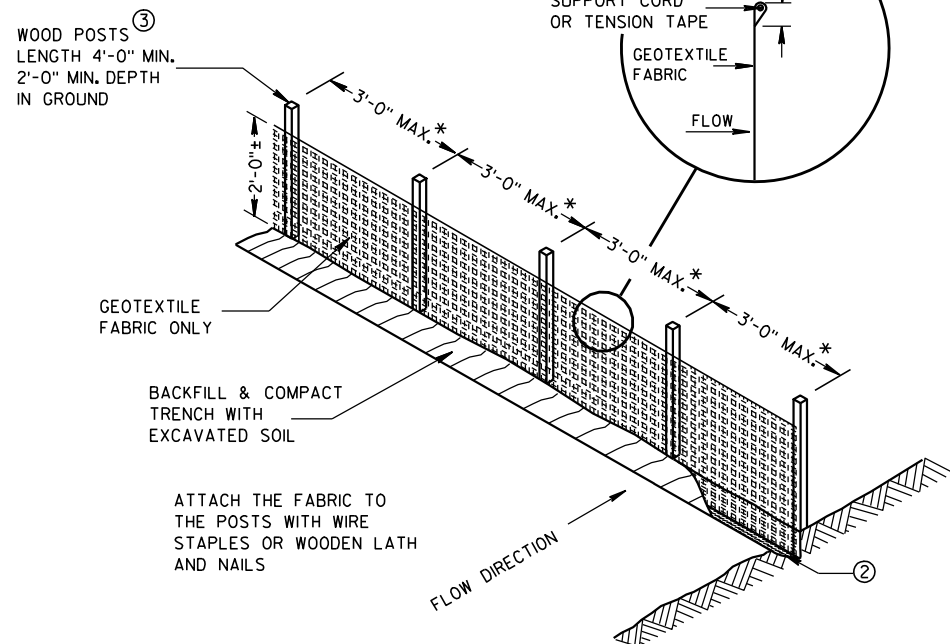
DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND APPLICABLE SPECIAL PROVISIONS.

- ① HORIZONTAL BRACE REQUIRED WITH 2" X 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② FOR MANUAL INSTALLATIONS THE TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- ③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 1 1/8" X 1 1/8" OF OAK OR HICKORY.
- ④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- ⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS; A) OVERLAP THE END POSTS AND TWIST, OR ROTATE, AT LEAST 180 DEGREES, B) HOOK THE END OF EACH SILT FENCE LENGTH.



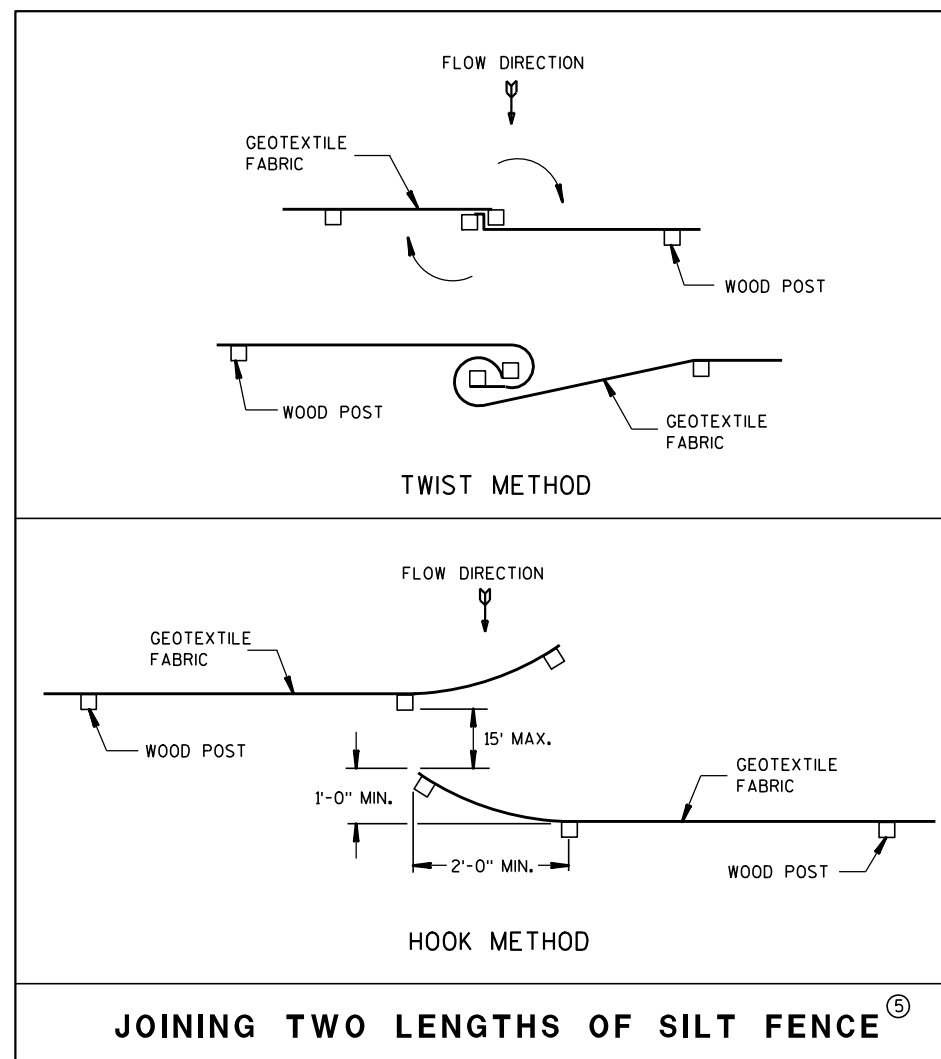
TRENCH DETAIL

NOTE: ADDITIONAL POST DEPTH OR TIE BACKS MAY BE REQUIRED IN UNSTABLE SOILS

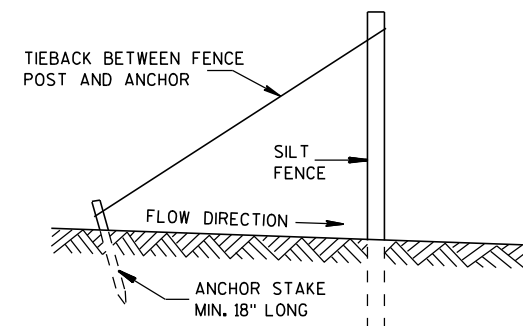


SILT FENCE

* NOTE: 8'-0" POST SPACING ALLOWED IF A WOVEN GEOTEXTILE FABRIC IS USED.



JOINING TWO LENGTHS OF SILT FENCE ⑤

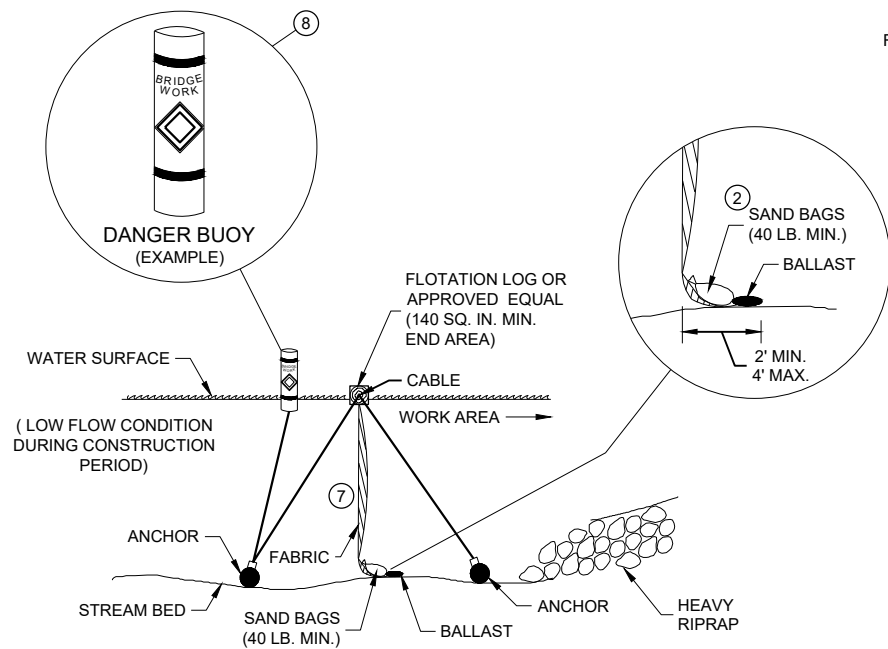


SILT FENCE TIE BACK
(WHEN REQUIRED BY THE ENGINEER)

SILT FENCE

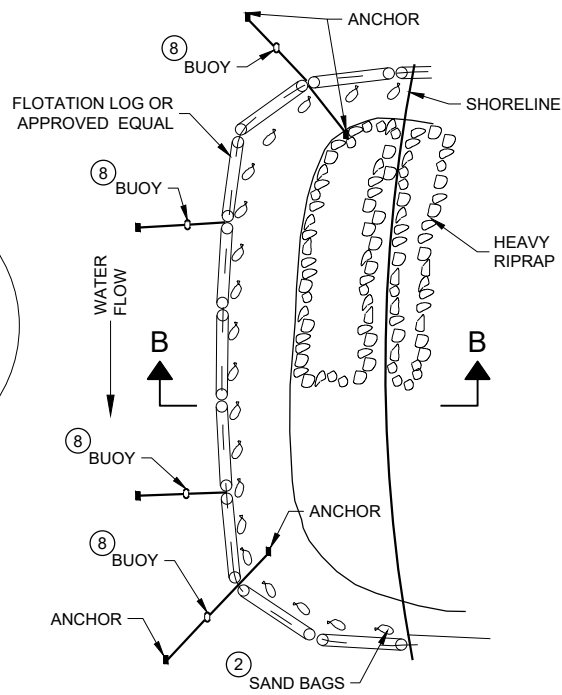
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
4-29-05 /S/ Beth Canestra
DATE CHIEF ROADWAY DEVELOPMENT ENGINEER
FHWA

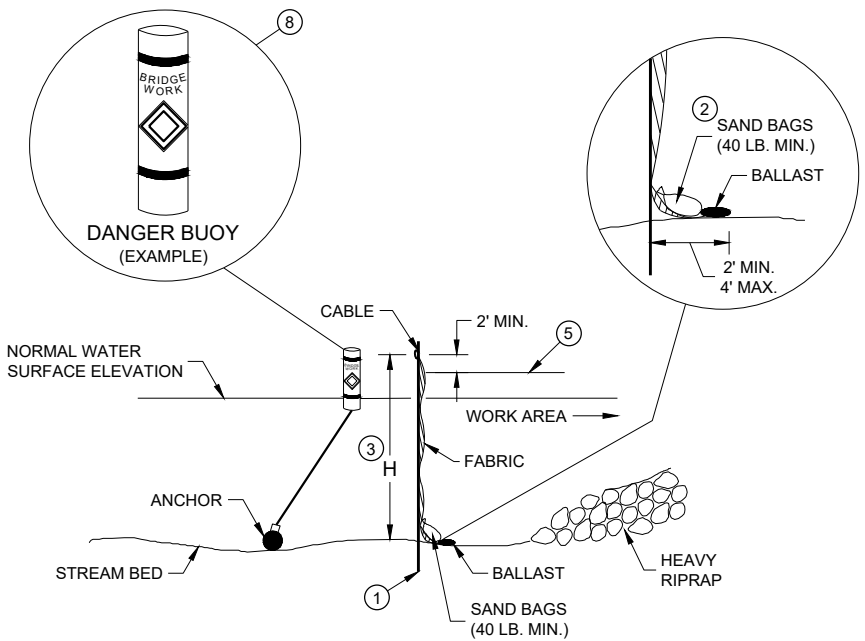


SECTION B - B

**TURBIDITY BARRIER - FLOAT ALTERNATIVE
CAUTION - SEE NOTE 6**

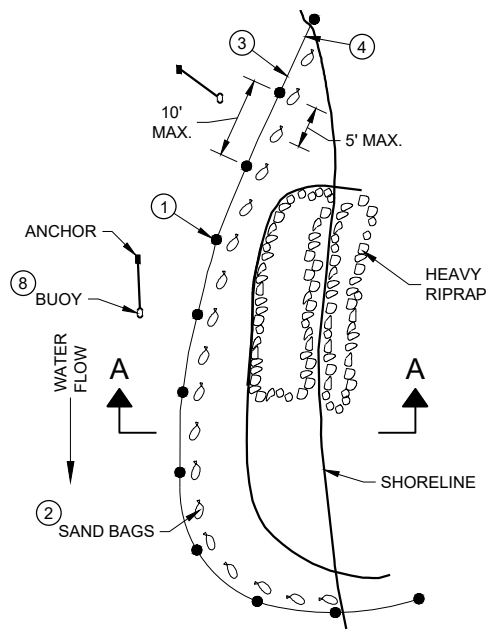


PLAN VIEW



SECTION A - A

TURBIDITY BARRIER - STANDARD POST INSTALLATION



PLAN VIEW

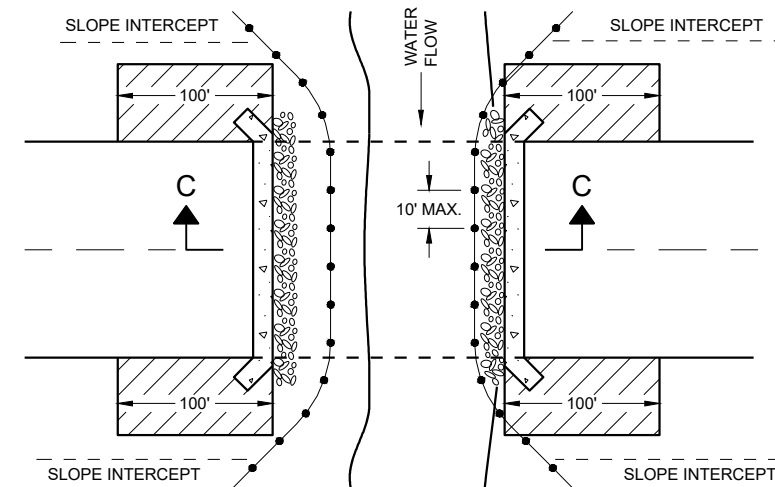
TURBIDITY BARRIER PLACEMENT DETAILS

GENERAL NOTES

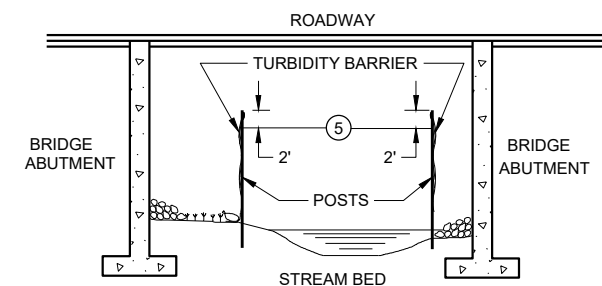
DETAILS OF CONSTRUCTION, MATERIALS AND WORKMANSHIP NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.

TURBIDITY BARRIER MAY BE REMOVED AT THE ENGINEERS DISCRETION, WHEN PERMANENT EROSION CONTROL MEASURES HAVE BEEN ESTABLISHED.

- ① DRIVEN STEEL POSTS, PIPES, OR CHANNELS. LENGTH SHALL BE SUFFICIENT TO SECURELY SUPPORT BARRIER AT HIGH WATER ELEVATIONS.
- ② SAND BAGS TO BE USED AS ADDITIONAL BALLAST WHEN ORDERED BY THE ENGINEER TO MEET ADVERSE FIELD CONDITIONS. SPACE AS APPROPRIATE FOR SITE CONDITIONS.
- ③ WHEN BARRIER HEIGHT "H" EXCEEDS 8 FEET, POST SPACING MAY NEED TO BE DECREASED.
- ④ IN WATERWAYS SUBJECT TO FLUCTUATING WATER ELEVATIONS, PROVISIONS SHOULD BE MADE TO ALLOW THE WATER TO EQUALIZE ON EACH SIDE OF THE BARRIER. THIS MAY BE ACCOMPLISHED BY LEAVING A PORTION OF THE BARRIER OPEN ON THE UPSTREAM END.
- ⑤ ESTIMATED HIGH WATER ELEVATION DURING CONSTRUCTION PERIOD. MINIMUM BARRIER HEIGHT SHALL BE 2' GREATER THAN EITHER THE Q2 ELEVATION OR THE ESTIMATED HIGH WATER ELEVATION DURING CONSTRUCTION, WHICHEVER IS GREATER.
- ⑥ FLOAT ALTERNATIVE WILL ONLY BE ALLOWED WITH WRITTEN APPROVAL OF THE ENGINEER, AND IS MEANT FOR LOCATIONS WHERE BEDROCK PREVENTS THE INSTALLATION OF POSTS.
- ⑦ ALLOW SUFFICIENT SLACK VERTICALLY AND HORIZONTALLY SO THAT SEDIMENT BUILD UP WILL NOT SEPARATE OR LOWER THE TURBIDITY BARRIER.
- ⑧ USE AS DIRECTED BY COAST GUARD OR DNR PERMIT WHEN WORKING IN NAVIGABLE WATERWAYS.



PLAN VIEW



SECTION C - C

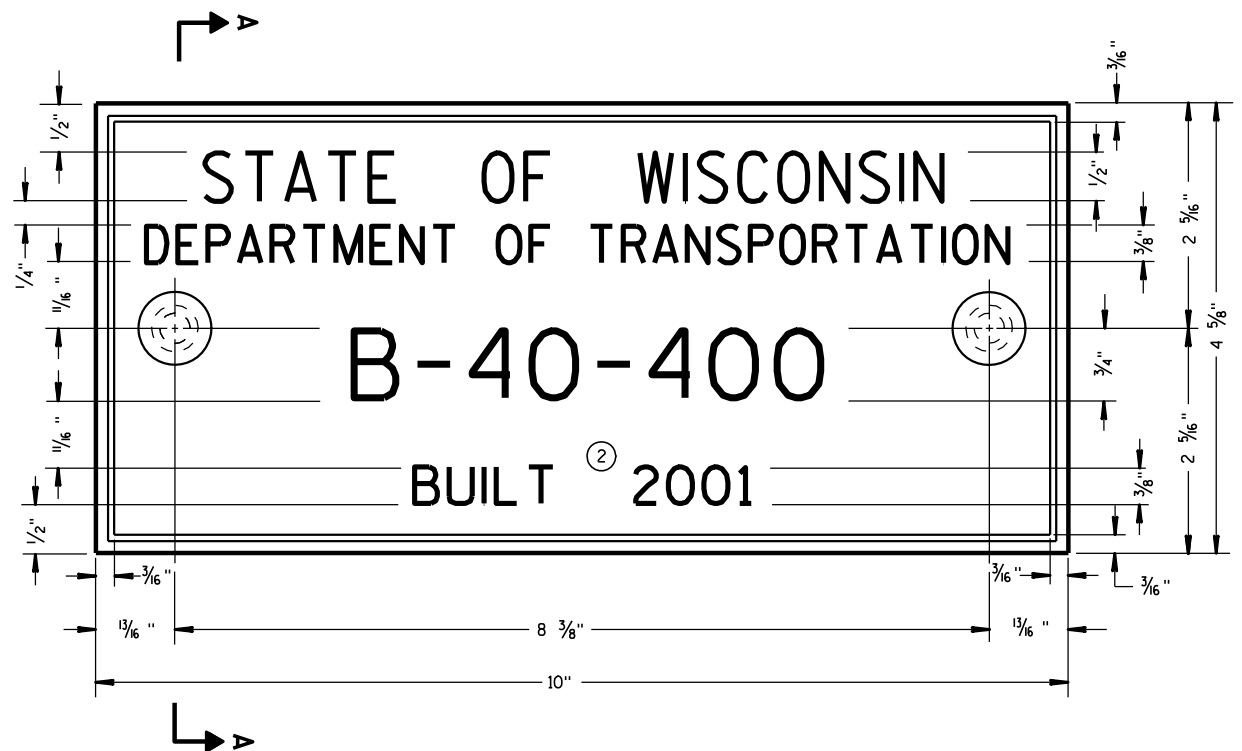
**TURBIDITY BARRIER DETAIL SHOWING
TYPICAL PLACEMENT AT STRUCTURES**

TURBIDITY BARRIER

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
6/4/02 DATE /S/ Beth Cannestra
DATE CHIEF ROADWAY DEVELOPMENT ENGINEER

FHWA



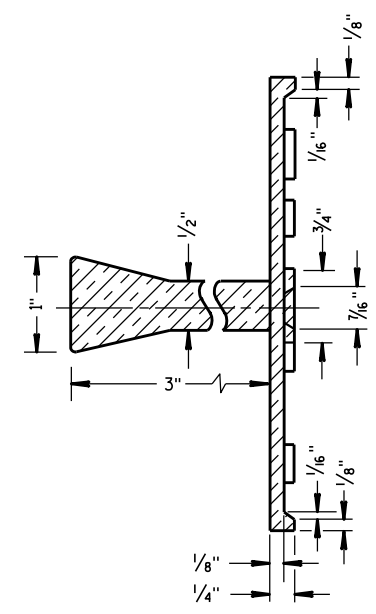
TYPICAL NAME PLATE
(BRIDGES, CULVERTS, AND RETAINING WALLS)

GENERAL NOTES

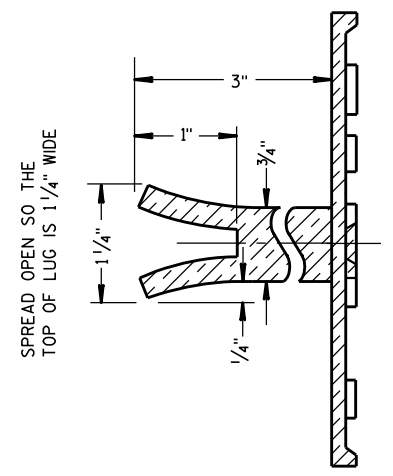
NAME PLATES TO BE INSTALLED ON BRIDGES, CULVERTS, AND RETAINING WALLS SHALL CONFORM TO THE REQUIREMENTS OF SECTION 502.3.11 OF THE STANDARD SPECIFICATIONS.

THE BRIDGE NUMBER AND YEAR BUILT SHOWN ON THIS DRAWING ARE EXAMPLES ONLY. SEE CONSTRUCTION PLANS FOR INDIVIDUAL NUMBERING AND YEAR BUILT.

- ① EPOXY RESIN SHALL BE FROM AN APPROVED MANUFACTURER AND USED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS.
- ② REHABILITATION OF AN EXISTING STRUCTURE SHOULD USE THE DATE OF ORIGINAL STRUCTURE CONSTRUCTION.



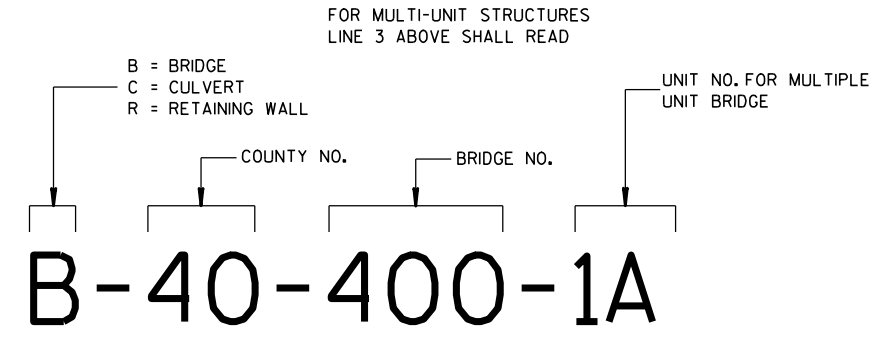
SECTION A-A



ALTERNATE LUG

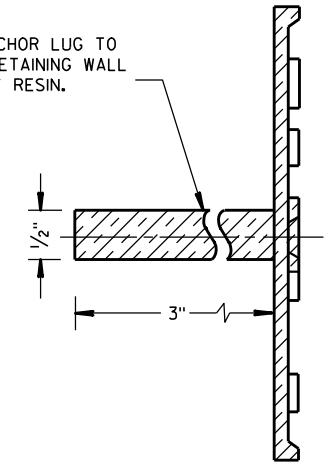
6

6



**NUMBERING DESIGNATION
MULTI-UNIT STRUCTURES**

- ① ADHERE ANCHOR LUG TO PRECAST RETAINING WALL WITH EPOXY RESIN.

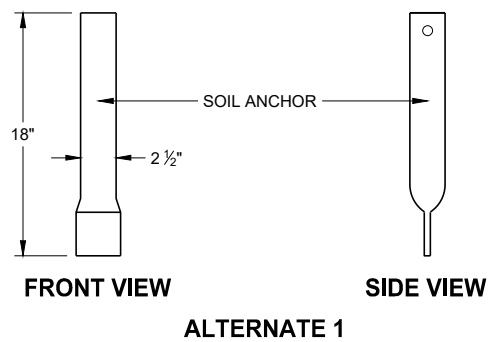
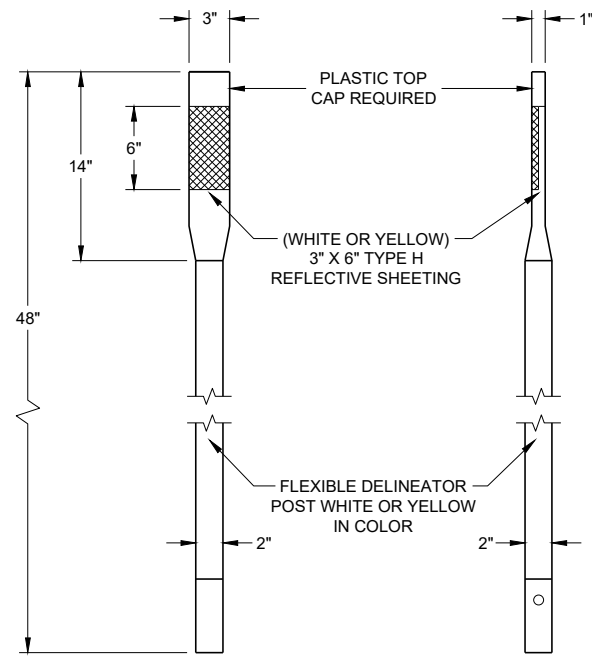


ALTERNATE LUG
(FOR ATTACHMENT TO PRECAST STRUCTURES)

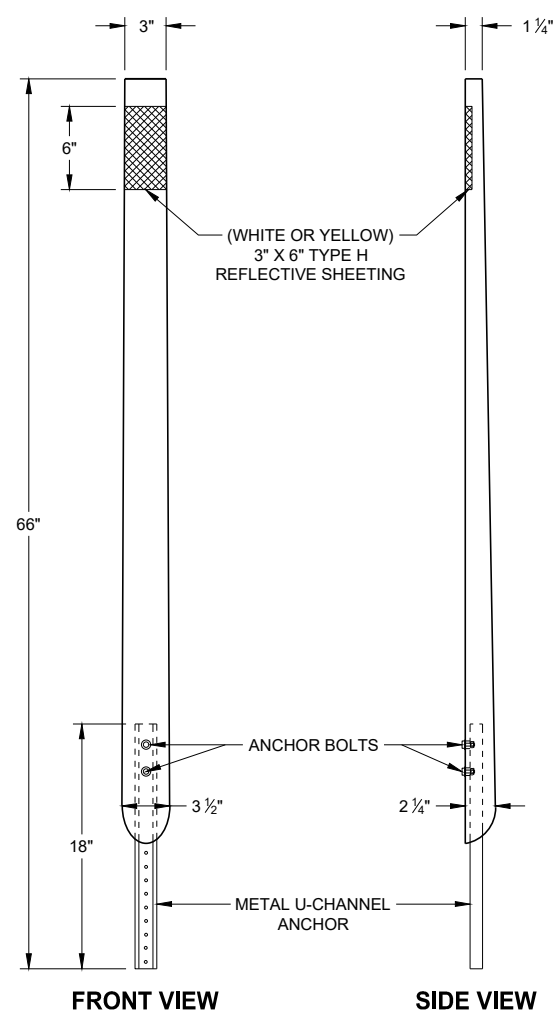
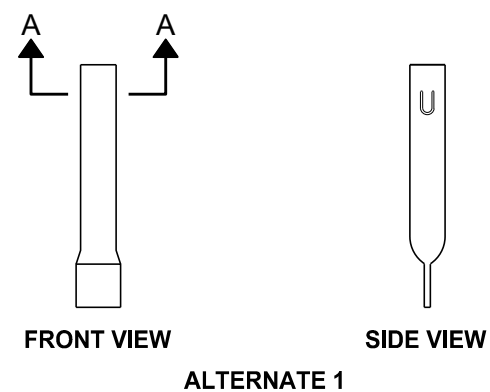
S.D.D. 12 A 3-10

S.D.D. 12 A 3-10

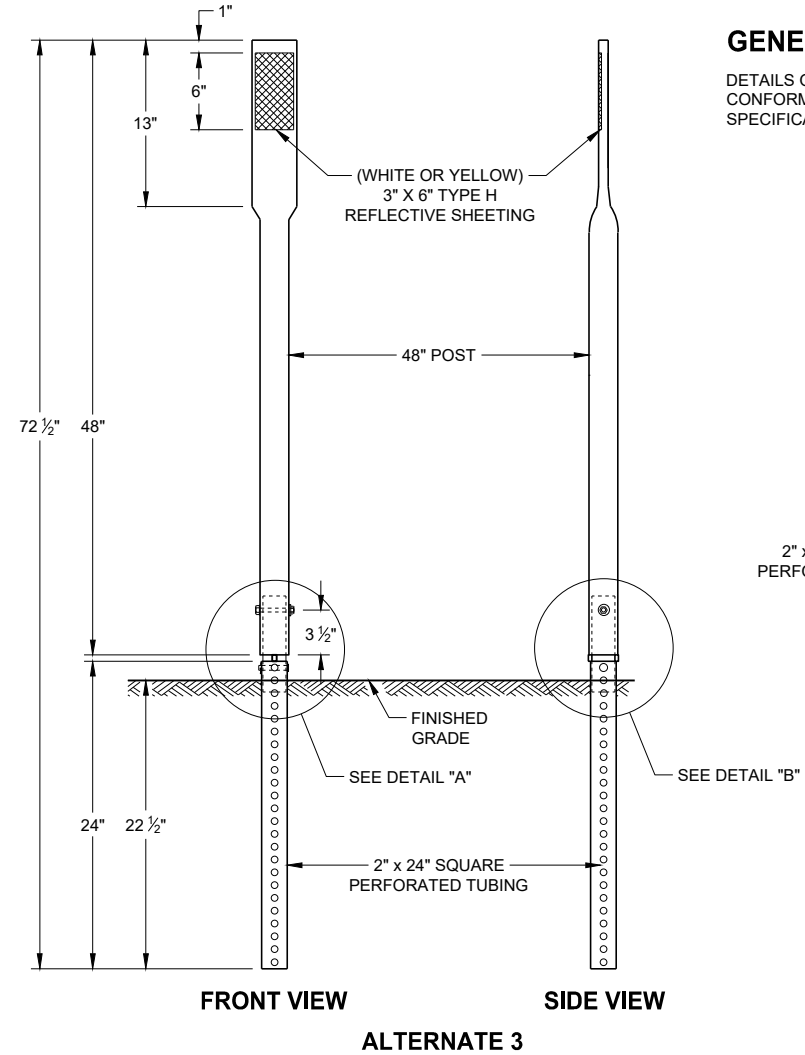
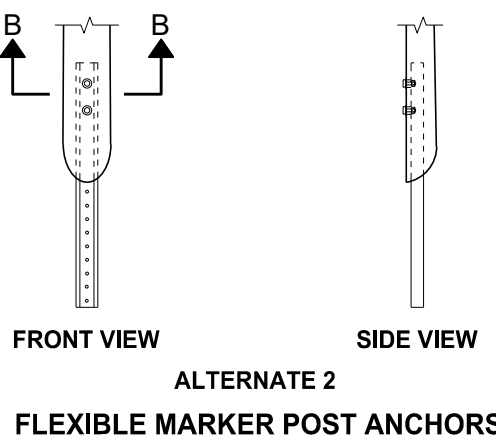
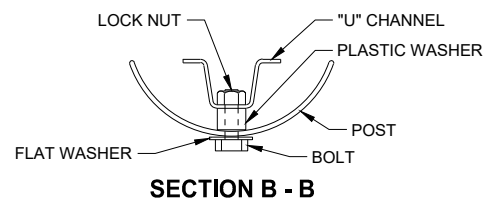
| | |
|----------------------------------------------------|----------------------------------------------------------|
| NAME PLATE (STRUCTURES) | |
| STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION | |
| APPROVED DATE 3/26/10 | /S/ Scot Becker CHIEF STRUCTURAL DEVELOPMENT ENGINEER |
| FHWA | |



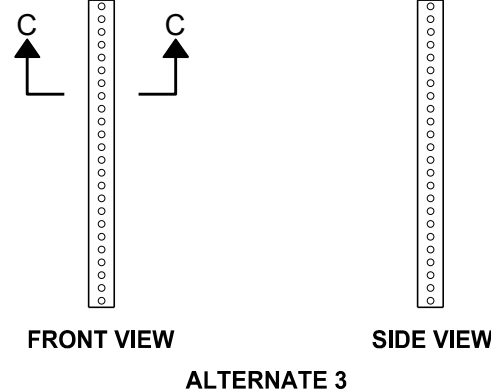
SECTION A - A



FLEXIBLE DELINEATOR POSTS

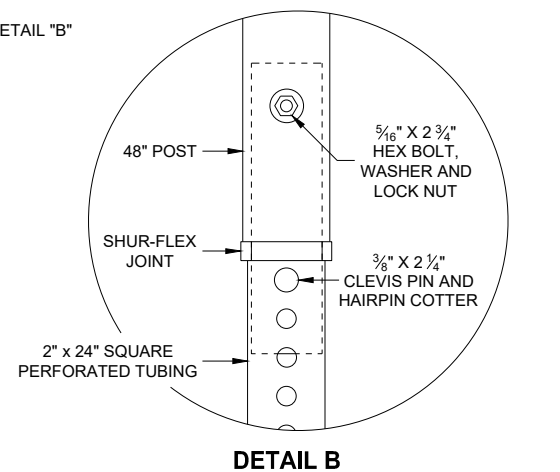
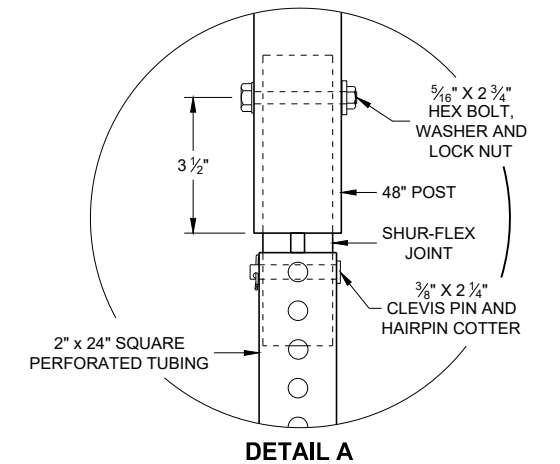


SECTION C - C



GENERAL NOTES

DETAILS OF CONSTRUCTION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS AND THE APPLICABLE SPECIAL PROVISIONS.



REFLECTOR SPACING TABLE

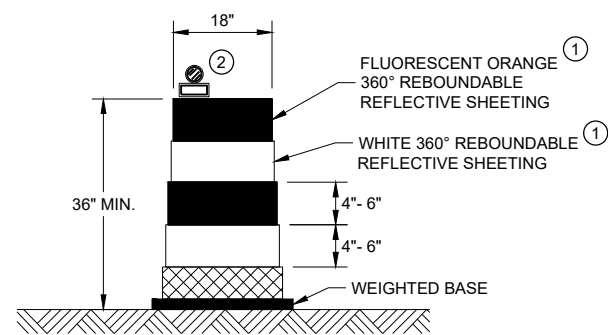
| REFLECTOR SPACING | LOCATION |
|-------------------|----------|
| * 100' C-C | RAMPS |
| 400' C-C | MAINLINE |

* START AT BEGINNING OF RAMP TAPER AND END AT END OF RAMP TAPER

FLEXIBLE DELINEATOR POST

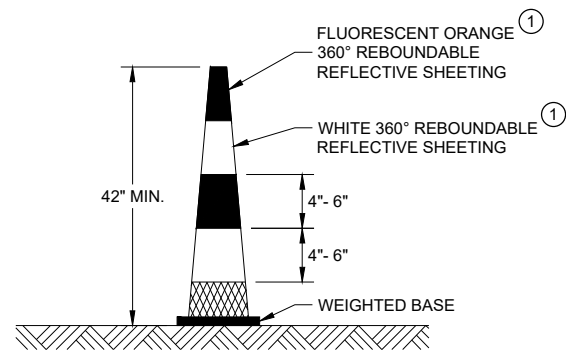
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2021 /S/ Matthew Rauch
DATE STATE SIGNING AND MARKING
ENGINEER
FHWA



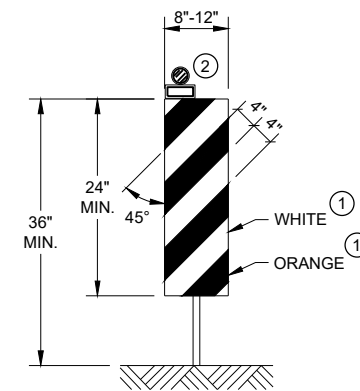
DRUM

BALLAST WIDTHS
RANGE FROM 24"-36"



42" CONE

DO NOT USE IN TAPERS
1/2 SPACING OF DRUMS
BALLAST WIDTHS
RANGE FROM 14"-20"

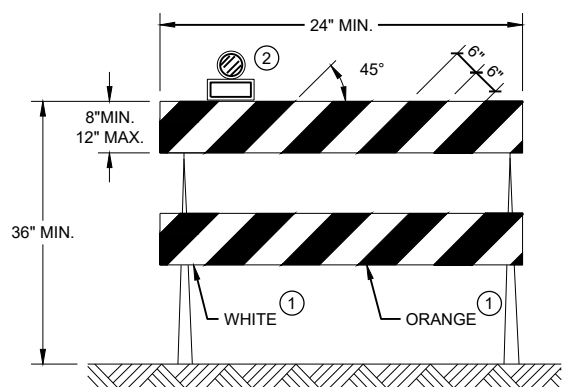


VERTICAL PANEL

THE STRIPES SHALL SLOPE DOWNWARD TO
THE TRAFFIC SIDE FOR CHANNELIZATION.

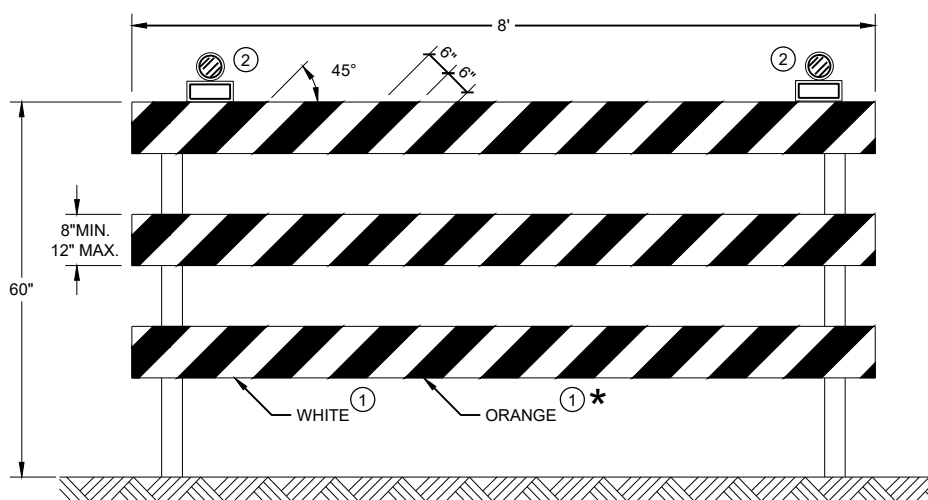
GENERAL NOTES

- ① REFLECTIVE SHEETING SHALL FOLLOW THE REQUIREMENTS IN THE APPROVED PRODUCTS LISTING FOR SIGN SHEETING.
- ② LOCATION OF WARNING LIGHTS WHEN SHOWN ON THE PLAN.



TYPE II BARRICADE

FOR RAILS LESS THAN 36" LONG, 4" WIDE STRIPES
MAY BE USED. ALL STRIPES SHALL SLOPE DOWNWARD
TO THE TRAFFIC SIDE FOR CHANNELIZATION.



TYPE III BARRICADE

IF SIGN MOUNTED, DO NOT COVER MORE THAN 50% OF THE TOP
TWO RAILS OR 33% OF THE TOTAL AREA OF THE THREE RAILS.






* IF USED FOR A PERMANENT APPLICATION USE RED SHEETING.

**CHANNELIZING DEVICES
DRUMS, CONES, BARRICADES
AND VERTICAL PANELS**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED
November 2022 /S/ Andrew Heidtke
DATE WORK ZONE ENGINEER
FHWA

LEGEND

-  SIGN ON PORTABLE OR PERMANENT SUPPORT
-  TEMPORARY PORTABLE RUMBLE STRIP ARRAY
-  DIRECTION OF TRAFFIC
-  WORK AREA
-  FLAGGER, EQUIPPED WITH STOP/SLOW PADDLE FASTENED ON SUPPORT STAFF

GENERAL NOTES

DETAILS OF TRAFFIC CONTROL DEVICES AND INSTALLATION NOT SHOWN ON THIS DRAWING SHALL CONFORM TO THE PERTINENT REQUIREMENTS OF THE STANDARD SPECIFICATIONS, THE SPECIAL PROVISIONS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

ALL SIGNS ARE 48" X 48" UNLESS OTHERWISE NOTED.

"WO" SIGNS ARE THE SAME AS "W" SIGNS EXCEPT THE BACKGROUND IS ORANGE.

THE EXACT NUMBER, LOCATION AND SPACING OF ALL SIGNS, DEVICES, AND LOCATION OF ALL FLAGGERS SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE FIRST ADVANCE WARNING SIGN SHOULD TYPICALLY BE LOCATED IN ADVANCE OF THE ANTICIPATED TRAFFIC BACKUP OR QUEUE.

WHEN A SIDE ROAD OR RAMP INTERSECTS THE FACILITY ON WHICH THE WORK IS BEING PERFORMED, ADDITIONAL TRAFFIC CONTROLS SHALL BE PROVIDED AS SPECIFIED IN THE PLANS AND/OR THE SPECIAL PROVISIONS OR AS APPROVED BY THE ENGINEER.

FLAGGING

FLAGGERS SHALL BE IN SIGHT OF EACH OTHER OR IN DIRECT COMMUNICATION AT ALL TIMES. THEY SHALL BE EQUIPPED WITH STOP/SLOW PADDLES FASTENED ON SUPPORT STAFFS. WHEN THE FLAGGING OPERATION IS NOT IN EFFECT REMOVE TEMPORARY PORTABLE RUMBLE STRIPS PRIOR TO COVERING OR REMOVING ALL ADVANCE SIGNING.

- ① FOR MOVING WORK OPERATIONS, POST ADDITIONAL W20-7A FLAGGER SIGNS AT APPROXIMATELY 3,500' INTERVALS IN THE MOVING WORK OPERATION OR AS APPROVED BY THE ENGINEER.
- ② SIGN NOT REQUIRED IF FLAGGING OPERATION OCCURS WITHIN A SIGNED ROAD WORK ZONE AREA.

WHEN THE DISTANCE BETWEEN FLAGGERS EXCEEDS 2 MILES, A PILOT CAR IS REQUIRED. WHEN CURVES REDUCE SIGHT DISTANCE BELOW 400', A PILOT CAR IS REQUIRED.

TEMPORARY PORTABLE RUMBLE STRIPS

UTILIZE TEMPORARY PORTABLE RUMBLE STRIPS ON ALL FLAGGING OPERATIONS.

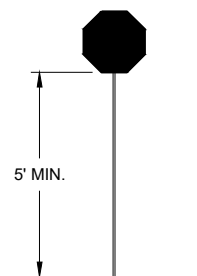
- ③ EACH TEMPORARY PORTABLE RUMBLE STRIP ARRAY CONSISTS OF THREE RUMBLE STRIPS PLACED TRANSVERSE ACROSS THE LANE AT THE LOCATIONS SHOWN. WITHIN EACH ARRAY, SPACING BETWEEN RUMBLE STRIPS SHALL BE 15 FEET ON CENTER

ONLY USE TEMPORARY PORTABLE RUMBLE STRIPS FROM THE APPROVED PRODUCTS LIST.

INSTALL TEMPORARY RUMBLE STRIPS PER MANUFACTURER'S RECOMMENDATIONS.

PLACE ADVANCE SIGNING PRIOR TO INSTALLING TEMPORARY RUMBLE STRIPS.

DO NOT INSTALL TEMPORARY PORTABLE RUMBLE STRIPS ON GRAVEL, MILLED SURFACES, OR ASPHALT THAT HAS BEEN PAVED LESS THAN 12 HOURS.



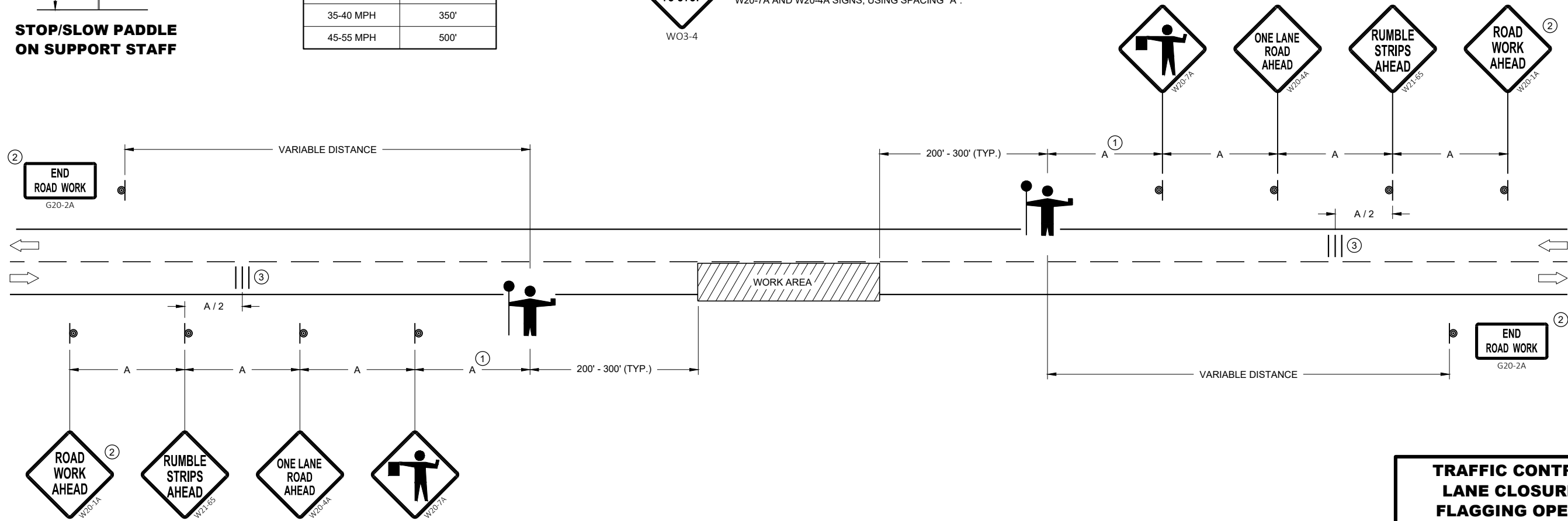
STOP/SLOW PADDLE ON SUPPORT STAFF

SIGN AND TEMPORARY RUMBLE STRIP ARRAY SPACING TABLE

| SPEED LIMIT | SPACING "A" |
|-------------|-------------|
| 25-30 MPH | 200' |
| 35-40 MPH | 350' |
| 45-55 MPH | 500' |



USE OF W03-4 SIGN IS OPTIONAL. WHEN USED, THIS SIGN SHALL BE LOCATED BETWEEN THE W20-7A AND W20-4A SIGNS, USING SPACING "A".



6

6

SDD 15C12 - 09a

SDD 15C12 - 09a













TRAFFIC CONTROL FOR LANE CLOSURE WITH FLAGGING OPERATION

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED _____ /S/ Andrew Heidtke
DATE May 2022 WORK ZONE ENGINEER

FHWA

LEGEND

-  SIGN ON PERMANENT SUPPORT
-  TRAFFIC CONTROL DRUM
-  TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
-  TEMPORARY DELINEATOR (WHITE, SINGLE DELINEATOR)
-  TYPE III BARRICADE
-  TYPE III BARRICADE WITH ATTACHED SIGN
-  TYPE "A" WARNING LIGHT (FLASHING)
-  TEMPORARY RAISED PAVEMENT MARKERS (TWO WAY YELLOW)
-  TEMPORARY STEEL PLATE BEAM GUARD AND END TREATMENT
-  DIRECTION OF TRAFFIC
-  REMOVE PAVEMENT MARKINGS
-  WORK AREA

GENERAL NOTES

ALL SIGNS ARE 48"X48" UNLESS OTHERWISE NOTED.

"WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.

ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH THE TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED AND AS APPROVED BY THE ENGINEER.

THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.

THE SPACING BETWEEN SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500' DESIRABLE) DISTANCE TO EXISTING SIGNS.

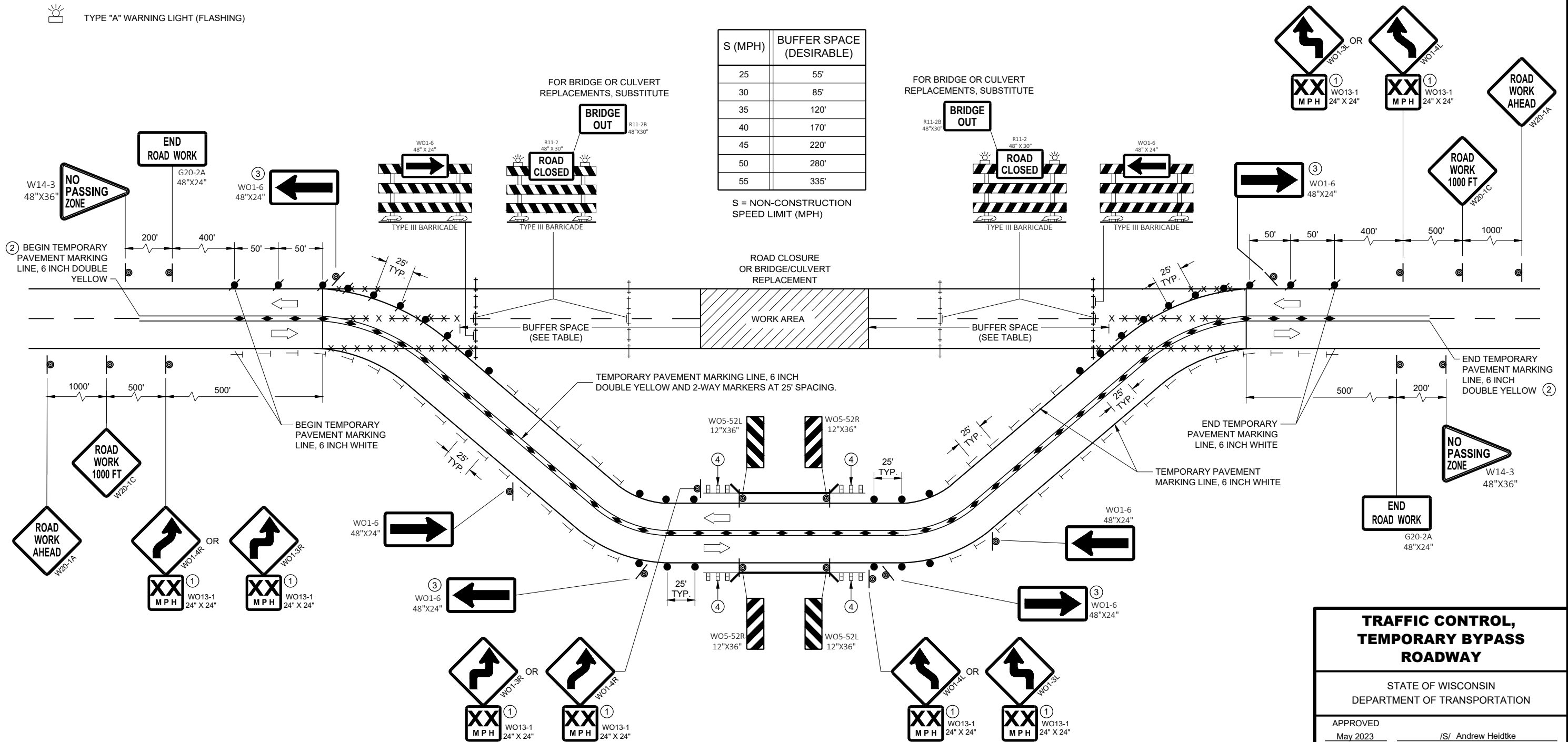
SIGN LAYOUTS SHALL BE IN ACCORDANCE WITH THE FHWA'S MANUAL ON STANDARD HIGHWAY SIGNS OR THE WISCONSIN STANDARD SIGN PLATES.

EQUIPMENT, VEHICLES, OR MATERIAL SHOULD NOT BE STORED IN BUFFER SPACE.

- ① IF ADVISORY SPEED IS GREATER THAN 30 MPH, USE THE WO1-4 SIGN. IF ADVISORY SPEED IS 30 MPH OR LESS, USE THE WO1-3 SIGN.
- ② WHEN THE DISTANCE TO / FROM THE NEXT CLOSEST NO-PASSING ZONE IS LESS THAN THE MINIMUM DISTANCE BETWEEN ZONES AS INDICATED IN THE SPECIFICATIONS, THE TWO ZONES SHALL BE CONNECTED.
- ③ OMIT THESE WO1-6 SIGNS IF THE ADVISORY SPEED OF THE CURVE IS GREATER THAN 30 MPH.
- ④ TEMPORARY STEEL PLATE BEAM GUARD AND END TREATMENT WHEN INCLUDED IN THE CONTRACT. FOR LAYOUT, SEE DETAILS ELSEWHERE IN THE PLAN.

| S (MPH) | BUFFER SPACE (DESIRABLE) |
|---------|--------------------------|
| 25 | 55' |
| 30 | 85' |
| 35 | 120' |
| 40 | 170' |
| 45 | 220' |
| 50 | 280' |
| 55 | 335' |

S = NON-CONSTRUCTION SPEED LIMIT (MPH)



**TRAFFIC CONTROL,
TEMPORARY BYPASS
ROADWAY**

STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED _____ /S/ Andrew Heidtke
 May 2023 DATE WORK ZONE ENGINEER

FHWA

6

6

SDD 15D31-05

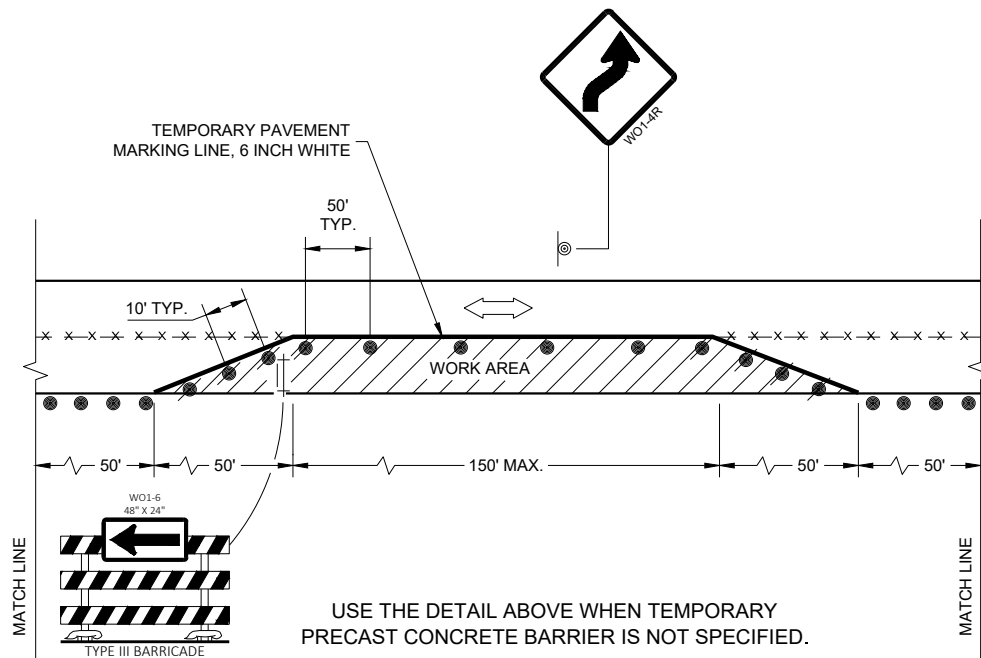
SDD 15D31-05

LEGEND

- TYPE III BARRICADE WITH ATTACHED SIGN
- SIGN ON PERMANENT SUPPORT
- TRAFFIC CONTROL DRUM WITH TYPE "C" STEADY BURN LIGHT
- TRAFFIC CONTROL DRUM
- FLAGS, 16" X 16" MIN. (ORANGE)
- REMOVING PAVEMENT MARKING
- DIRECTION OF TRAFFIC
- ASPHALTIC PAVEMENT WIDENING
- CONCRETE BARRIER TEMPORARY PRECAST

GENERAL NOTES

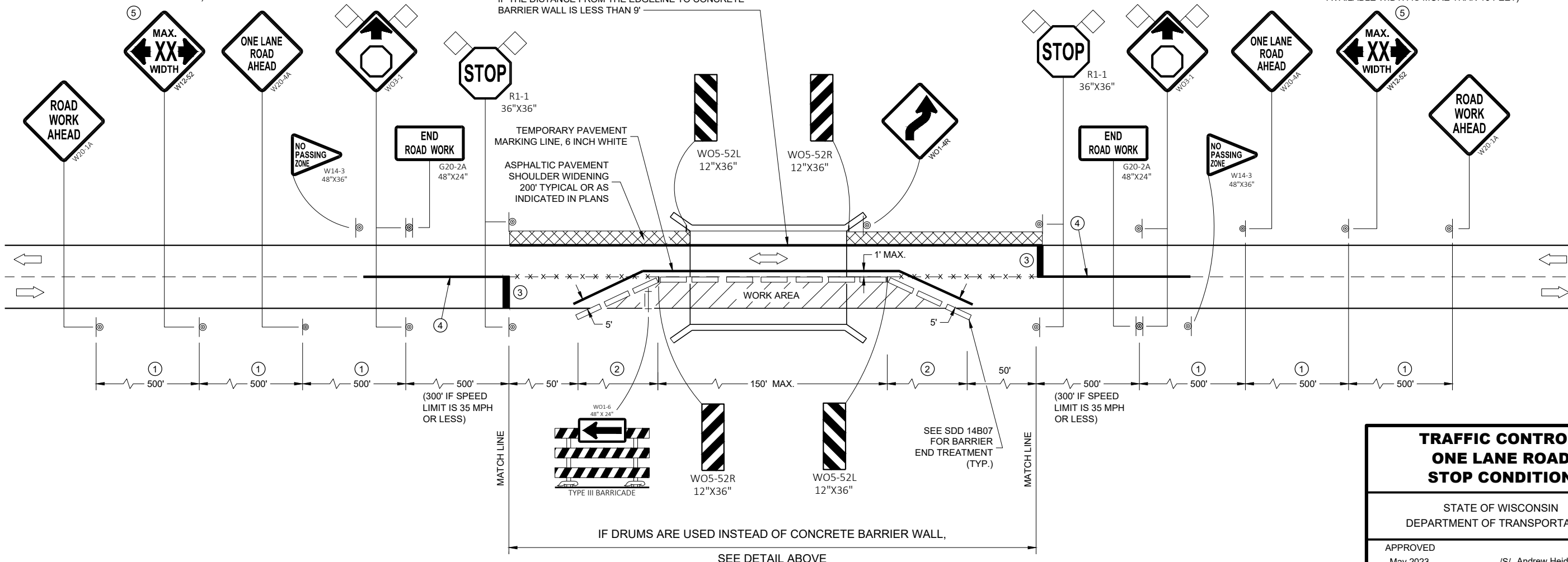
- THE EXACT NUMBER, LOCATION, AND SPACING OF ALL SIGNS AND DEVICES SHALL BE ADJUSTED TO FIT FIELD CONDITIONS AS APPROVED BY THE ENGINEER.
- THE SPACING BETWEEN TRAFFIC CONTROL SIGNS SHOULD BE ADJUSTED TO NOT CONFLICT WITH AND TO PROVIDE A MINIMUM OF 200 FEET (500 FEET DESIRABLE) CLEARANCE TO EXISTING SIGNS THAT WILL REMAIN IN PLACE..
- THIS LANE CLOSURE IS TYPICAL FOR CLOSING RIGHT LANE - REVERSE FOR CLOSING LEFT LANE.
- ALL SIGNS ARE 48" x 48" UNLESS OTHERWISE NOTED.
- "WO" IS THE SAME AS "W" EXCEPT THE BACKGROUND IS ORANGE.
- ANY SIGNS TEMPORARY OR EXISTING, WHICH CONFLICT WITH TRAFFIC CONTROL "IN USE" SHALL BE REMOVED OR COVERED AS NEEDED OR AS APPROVED BY THE ENGINEER.
- REMOVE PAVEMENT MARKINGS AND PLACE TEMPORARY PAVEMENT MARKING LINES IF LANE CLOSURE IS TO BE IN PLACE FOR 4 OR MORE CONTINUOUS DAYS AND NIGHTS.
- ① 500 FOOT SPACING SHOWN IS FOR ROADWAYS WITH A PRE-CONSTRUCTION REGULATORY SPEED LIMIT OF 45 MPH OR MORE. FOR 35 - 40 MPH, USE 350 FOOT TYPICAL SPACING. FOR 25 - 30 MPH, USE 200 FOOT TYPICAL SPACING.
 - ② DIMENSION DETERMINED BY CBTP TAPER FROM EDGE LINE TO TANGENT SECTION OF THE ROAD.
 - ③ TEMPORARY PAVEMENT MARKING LINE, 18 INCH WHITE STOP LINE.
 - ④ 700 FOOT TEMPORARY PAVEMENT MARKING LINE, 6 INCH DOUBLE YELLOW. WHEN THE DISTANCE FOR THE PRECEDING NO - PASSING ZONE IS LESS THAN THE MINIMUM DISTANCE BETWEEN ZONES AS INDICATED IN THE SPECIFICATIONS, THE TWO ZONES SHALL BE CONNECTED.
 - ⑤ SEE SDD 15C02 - SHEET "F" FOR ADVANCED WIDTH RESTRICTION SIGNING.



WIDTH ON SIGN TO BE APPROX. 1-FOOT LESS THAN AVAILABLE WIDTH. (OMIT IF AVAILABLE WIDTH IS MORE THAN 16 FEET)

TEMPORARY PAVEMENT MARKING LINE, 6 INCH WHITE, (STOPLINE TO STOPLINE), REMOVE EXISTING EDGELINE AND OFFSET THE TEMPORARY EDGELINE IF THE DISTANCE FROM THE EDGELINE TO CONCRETE BARRIER WALL IS LESS THAN 9'

WIDTH ON SIGN TO BE APPROX. 1-FOOT LESS THAN AVAILABLE WIDTH. (OMIT IF AVAILABLE WIDTH IS MORE THAN 16 FEET)



**TRAFFIC CONTROL,
ONE LANE ROAD
STOP CONDITION**

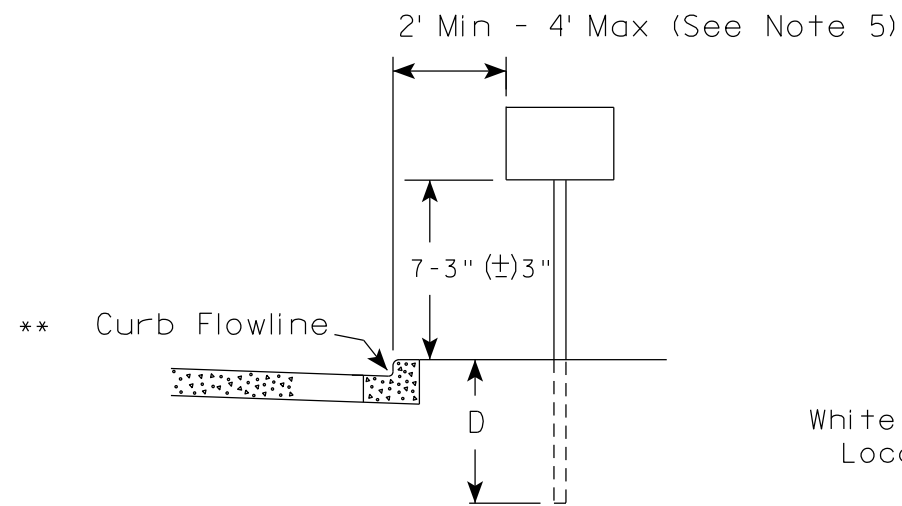
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

APPROVED _____ /S/ Andrew Heidtke
DATE _____
WORK ZONE ENGINEER

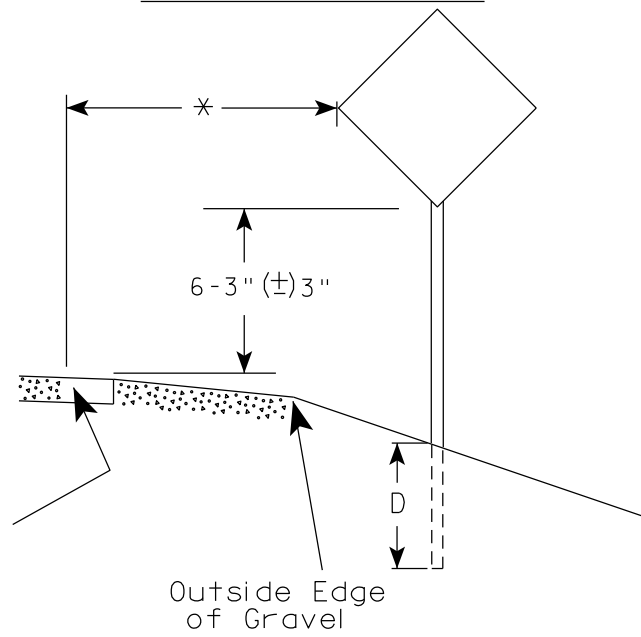
FHWA

URBAN AREA

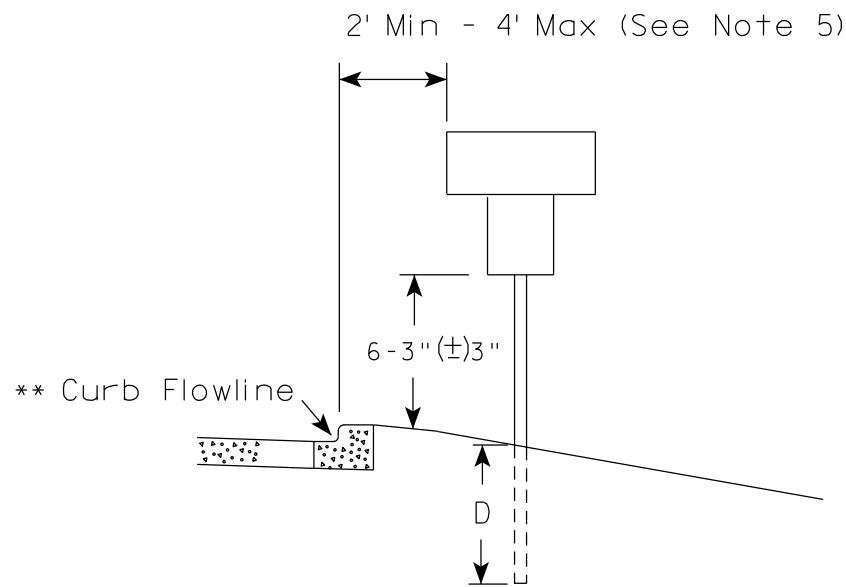
RURAL AREA (See Note 2)



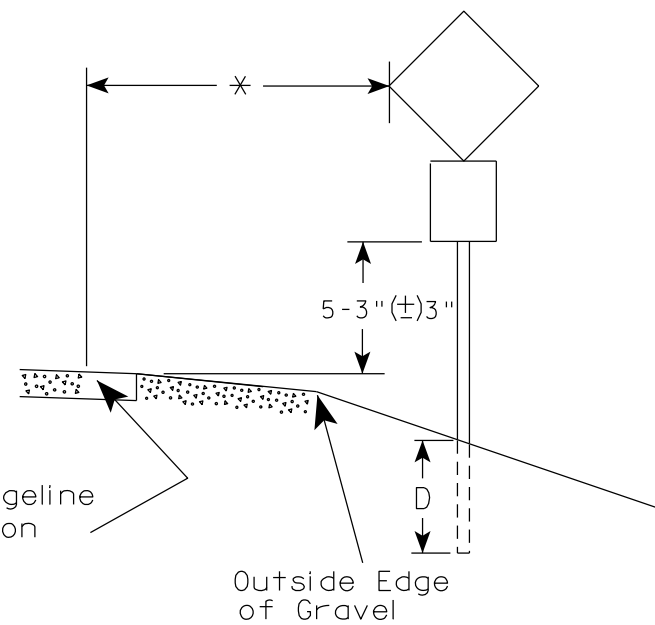
White Edgeline Location



Outside Edge of Gravel



White Edgeline Location



Outside Edge of Gravel

GENERAL NOTES

1. Signs wider than 4 feet or 20 sq.ft or larger, shall be mounted on multiple posts. Refer to plate A4-4.
2. If signs are mounted on or behind barrier wall, see A4-10 sign plate.
The Double Arrow sign (W12-1D) shall be mounted at a height of 2'-3" (± 3"). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Enhanced Reference Markers, Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (± 3").
3. For expressways and freeways, mounting height is 7'- 3" (± 3)" or 6'-3" (± 3)" depending upon existence of a sub-sign.
4. Minimum mounting height for signs mounted on traffic signal poles is 5'- 3" (± 3)".
5. Offset distance shall be consistent with existing signs or consistent throughout length of project.
6. Folding signs shall be mounted at a height of 5'-3" (± 3)" or as directed by the Engineer.

POST EMBEDMENT DEPTH

| Area of Sign Installation (Sq. Ft.) | D (Min) |
|---------------------------------------|-----------|
| 20 or Less | 4' |
| Greater than 20 | 5' |

* * The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

TYPICAL INSTALLATION OF PERMANENT TYPE II SIGNS ON SINGLE POSTS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED

Matthew R. Raub
for State Traffic Engineer

DATE 12/6/23

PLATE NO. A4-3.23



ELEVATION VIEW

DETAIL OF WOOD 4 X 6 SIGN POST IN BOX-OUT

- NOTES:**
1. ALL MATERIAL TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION
 2. SEE SIGN PLATE A4-8 FOR SIGN HARDWARE REQUIREMENTS
 3. 18 INCH X 18 INCH SQUARE BOX-OUTS MAY BE USED FOR INSTALLATIONS IN EXISTING CONCRETE OR ASPHALT LOCATIONS.



ELEVATION VIEW

DETAIL OF STEEL 2 X 2 SIGN POST IN BOX-OUT



PLAN VIEW

FOR NEW CONCRETE/ASPHALT INSTALLATIONS

| | |
|-------------------------------------------------------------------------------|----------------------------------|
| SIGN POST BOX-OUTS A4-3B | |
| <small>WISCONSIN DEPT OF TRANSPORTATION</small> | |
| APPROVED <i>Matthew R. Rauch</i> <small>for State Traffic Engineer</small> | |
| <small>DATE 1/27/14</small> | <small>PLATE NO. A4-3B.1</small> |

GENERAL NOTES

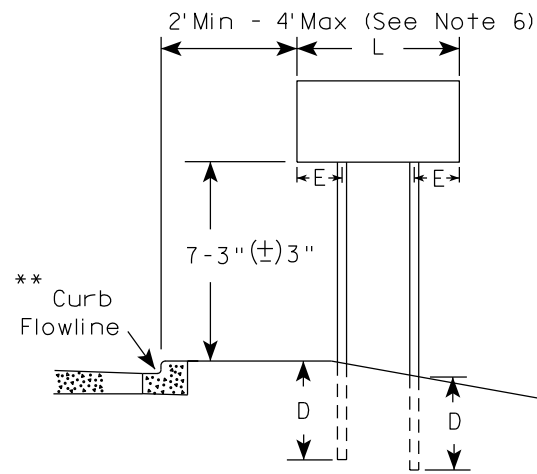
- For 3 or 4 post installations, individual post spacing shall be greater than 3'-6".
- See tables below for required number of posts.
- For expressways and freeways, mounting height is 7'-3" (\pm 3") or 6'-3" (\pm 3") depending upon existence of sub-sign.
- The (\pm) tolerance for mounting height is 3 inches.
- J-Assemblies are considered to be one sign for mounting height.
- Offset distance shall be consistent with existing signs or consistent throughout length of project.
- Folding signs shall be mounted at a height of 5'-3" (\pm 3") or as directed by the engineer.
- The Double Arrow sign (W12-1) shall be mounted at a height of 2'-3" (\pm 3"). The Chevron sign (W1-8), Roundabout Chevron panel (R6-4B), Clearance Markers (W5-52), Mile Markers (D10 series), In Road Object Markers (W5-54) & End of Road Markers (W5-56) shall be mounted at a height of 4'-3" (\pm 3").

* 6 feet from edge of a paved shoulder or 12 feet from the edge of pavement (edge line location) or 2 feet from outside edge of gravel, whichever is greater unless directed by project engineer.

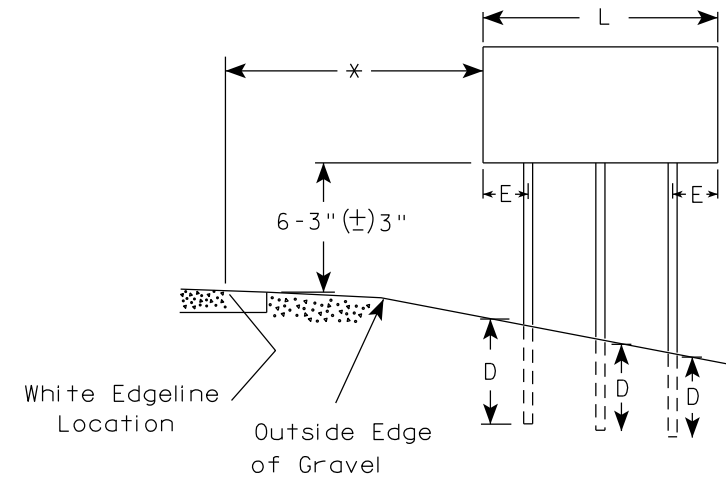
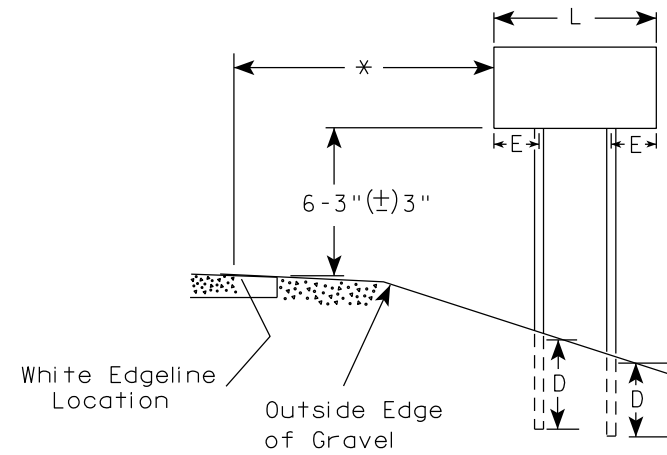
** The existence of curb and gutter does not in itself mandate the vertical clearance illustrated. That height is typically measured where there is sidewalk adjacent to the roadway or parking is permitted. In the absence of sidewalk vertical clearance is measured from the top of the curb. Offset of signs is measured from the flow line.

*** See A4-3 sign plate for signs 4' or less in width and less than 20 S.F. in area.

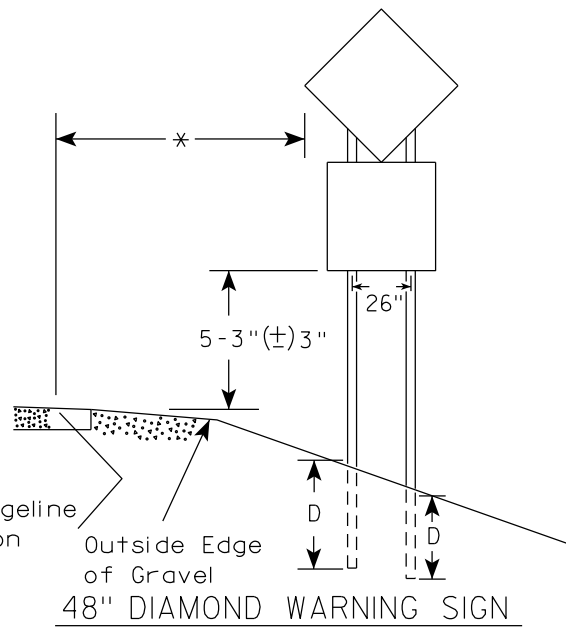
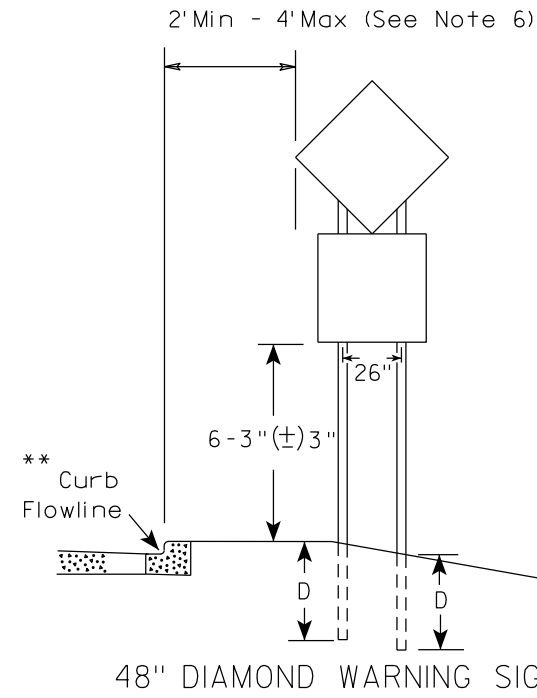
URBAN AREA



RURAL AREA (See Note 3)



URBAN AREA



| SIGN SHAPE OTHER THAN DIAMOND (TWO POSTS REQUIRED) | |
|----------------------------------------------------|-----|
| L | E |
| Greater than 48" Less than 60" | 12" |
| 60" to 108" | L/5 |

| SIGN SHAPE OTHER THAN DIAMOND (THREE POSTS REQUIRED) | |
|------------------------------------------------------|-----|
| L | E |
| Greater than 108" to 144" | 12" |

POST EMBEDMENT DEPTH

| Area of Sign Installation (Sq. Ft.) | D (Min) |
|---------------------------------------|-----------|
| 20 or Less | 4' |
| Greater than 20 | 5' |

TYPICAL INSTALLATION OF TYPE II SIGNS ON MULTIPLE POSTS

WISCONSIN DEPT OF TRANSPORTATION
 APPROVED *Matthew R. Rauch*
 For State Traffic Engineer
 DATE 12/6/23 PLATE NO. A4-4.16

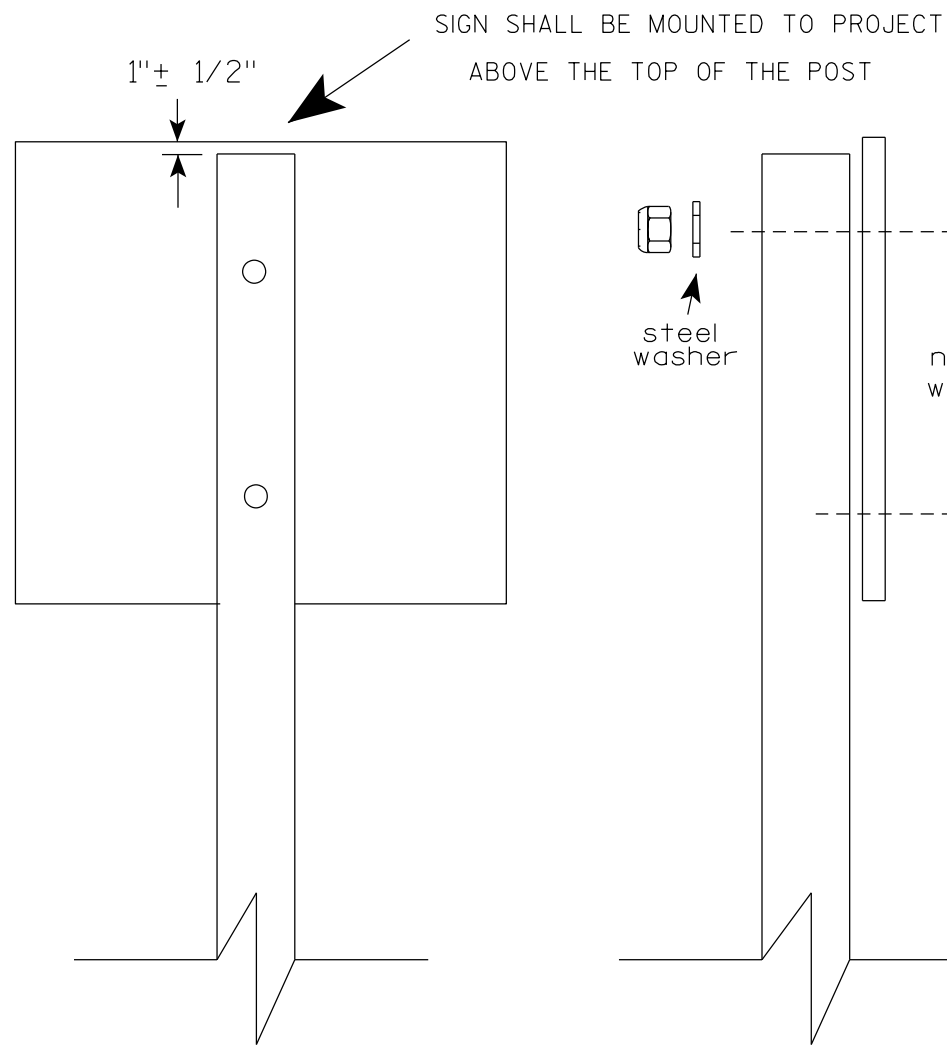
PROJECT NO:

HWY:

COUNTY:

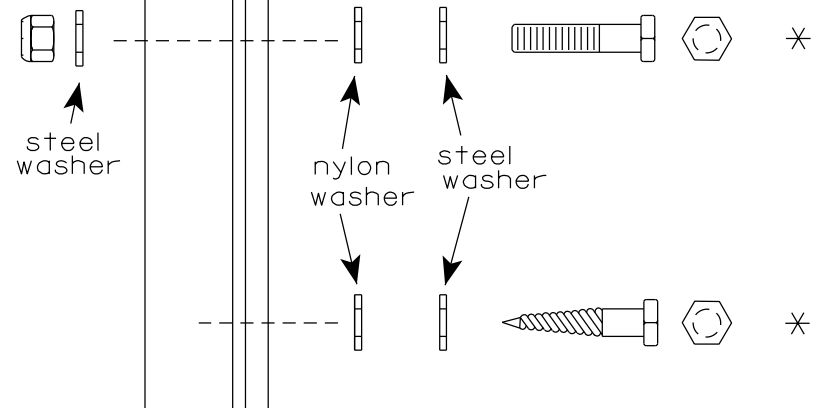
SHEET NO:

E



SIGN SHALL BE MOUNTED TO PROJECT ABOVE THE TOP OF THE POST

1"± 1/2"



Nuts, bolts and lags used for mounting signs shall have hexagonal heads and shall be either :

- Hot dip galvanized in accordance with ASTM Designation: A 153, Class D, or SC 3
- Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3.

Threads on bolts and nuts shall be manufactured with sufficient allowance for the cadmium plate or galvanized coating to permit the nuts to run freely on the bolts.

STRINGER BOLTING TO ALUMINUM SIGNS (SEE SIGN PLATE A4-18)

MACHINE BOLTS - 5/16" X 1-3/4" Length w/ lock nuts

WOOD POSTS (4" x 6")

LAG SCREWS - 3/8" X 3" (NO STRINGERS ON BACK OF SIGN)
3/8" X 4" (STRINGERS ON BACK OF SIGN)

SQUARE STEEL POSTS (2" x 2")

MACHINE BOLTS - 3/8" X 3-1/4" Length w/ nuts (NO STRINGER ON BACK OF SIGN)
3/8" X 5" Length w/ nuts (STRINGERS ON BACK OF SIGN)

RIVETS - 9/32" (6605-9-6) BULB-TITE, TRI-FOLD, ALUMINUM BODY/MANDREL
O.D. FLANGE .720-.765 INCH, GRIP RANGE .042-.375 INCH

WASHERS (ALL POSTS) -

1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL

1-1/4" O.D. X 3/8" I.D. X .080 NYLON

* Two different fastening systems are shown for illustration purposes. On any individual sign, either one or the other system shall be used. Actual number of fasteners per sign varies with the sign area, but normally there are two. For a single post installation, all signs greater than 9 sq. ft. require the use of 3 fasteners.

ATTACHMENT OF SIGNS
TO POSTS

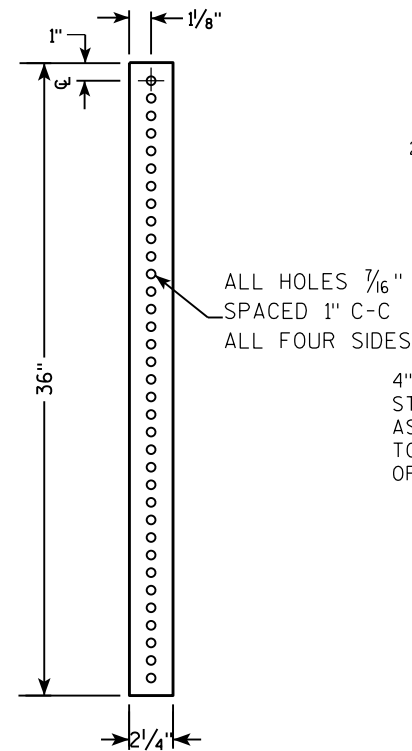
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R Rauch*
For State Traffic Engineer

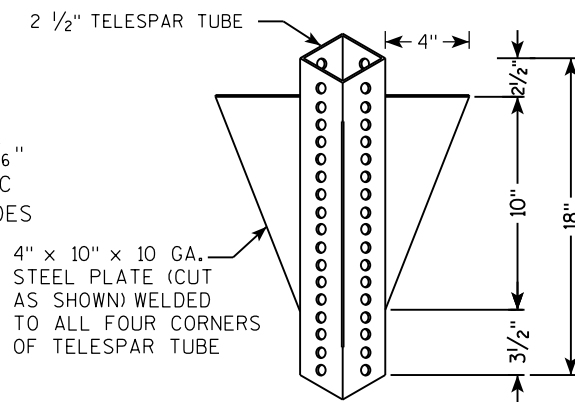
DATE 4/1/2020 PLATE NO. A4-8.9

**TELESCOPIC TUBING ANCHORS
TWO PIECE SYSTEM**

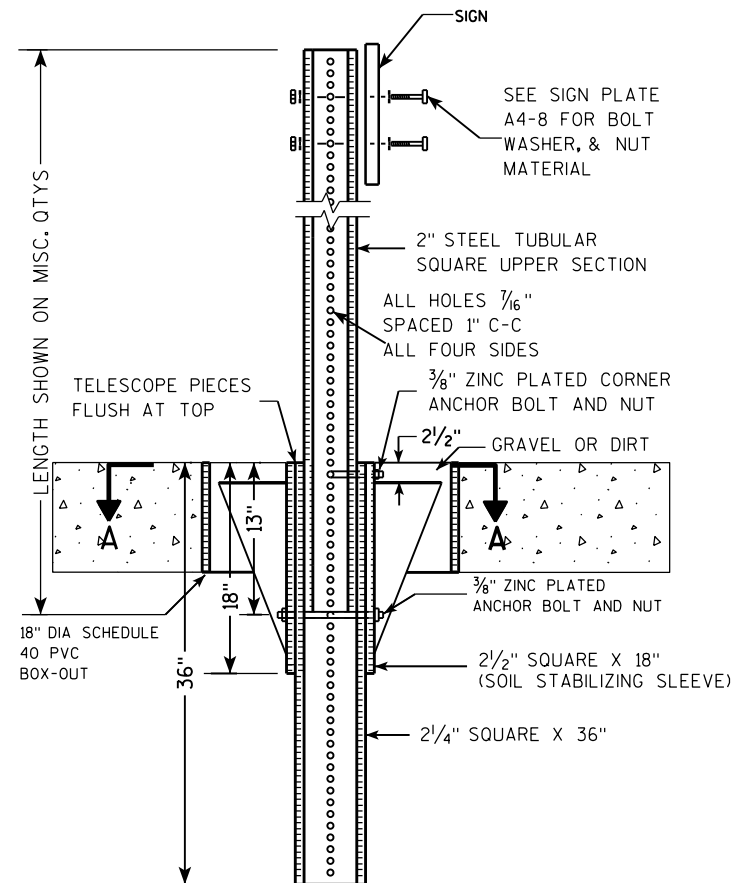
2 1/4" SQUARE
12 GAUGE
PERFORATED
GALVANIZED FINISH



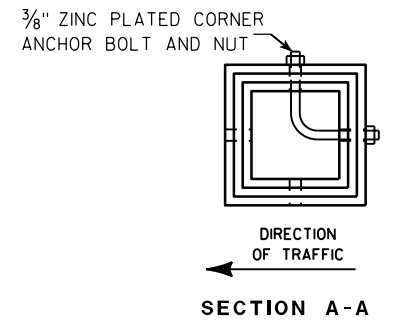
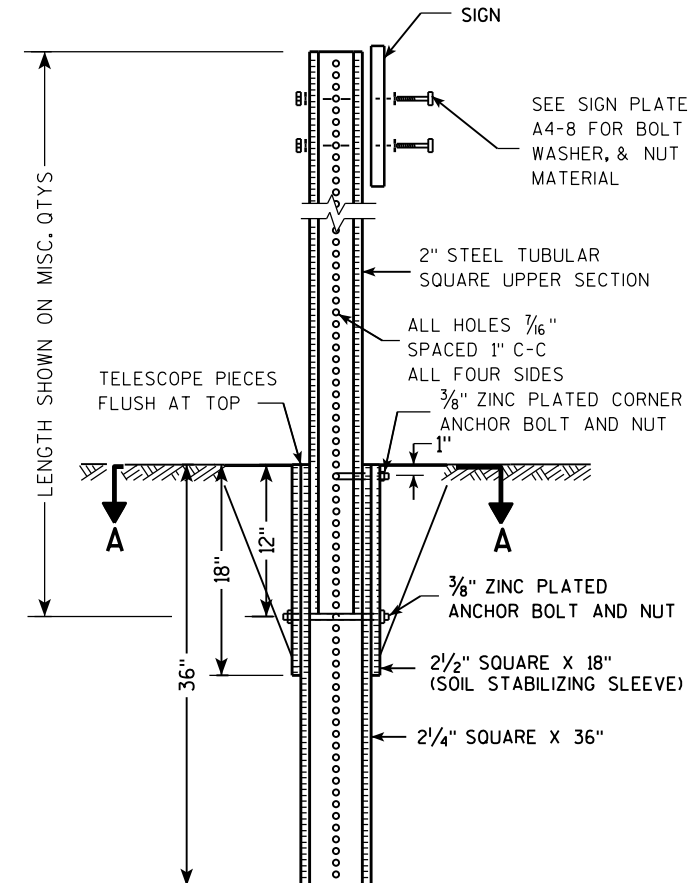
2 1/2" SQUARE
12 GAUGE
OMNI-DIRECTIONAL
PERFORATED
SOIL STABILIZING SLEEVE
GALVANIZED FINISH



**DETAIL OF TUBULAR STEEL SIGN POST
(IN POURED CONCRETE OR ASPHALT)**



**DETAIL OF TUBULAR STEEL SIGN POST
(IN LOCATIONS OTHER THAN POURED CONCRETE OR ASPHALT)**



| Area of Sign Installation (Sq. Ft.) | Number of Required Posts |
|------------------------------------------|--------------------------|
| 9 or less | 1 |
| Greater than 9 less than or equal to 18 | 2 |
| Greater than 18 less than or equal to 27 | 3 |

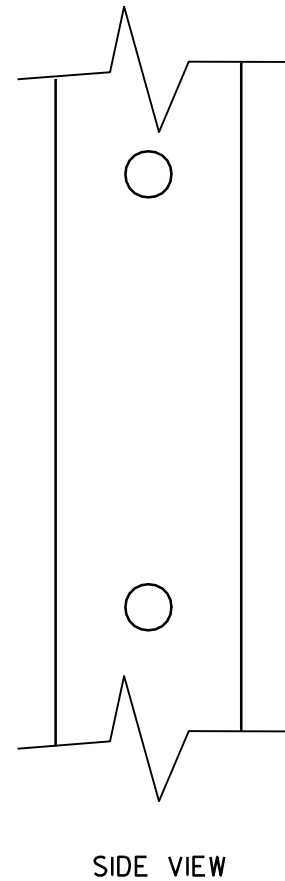
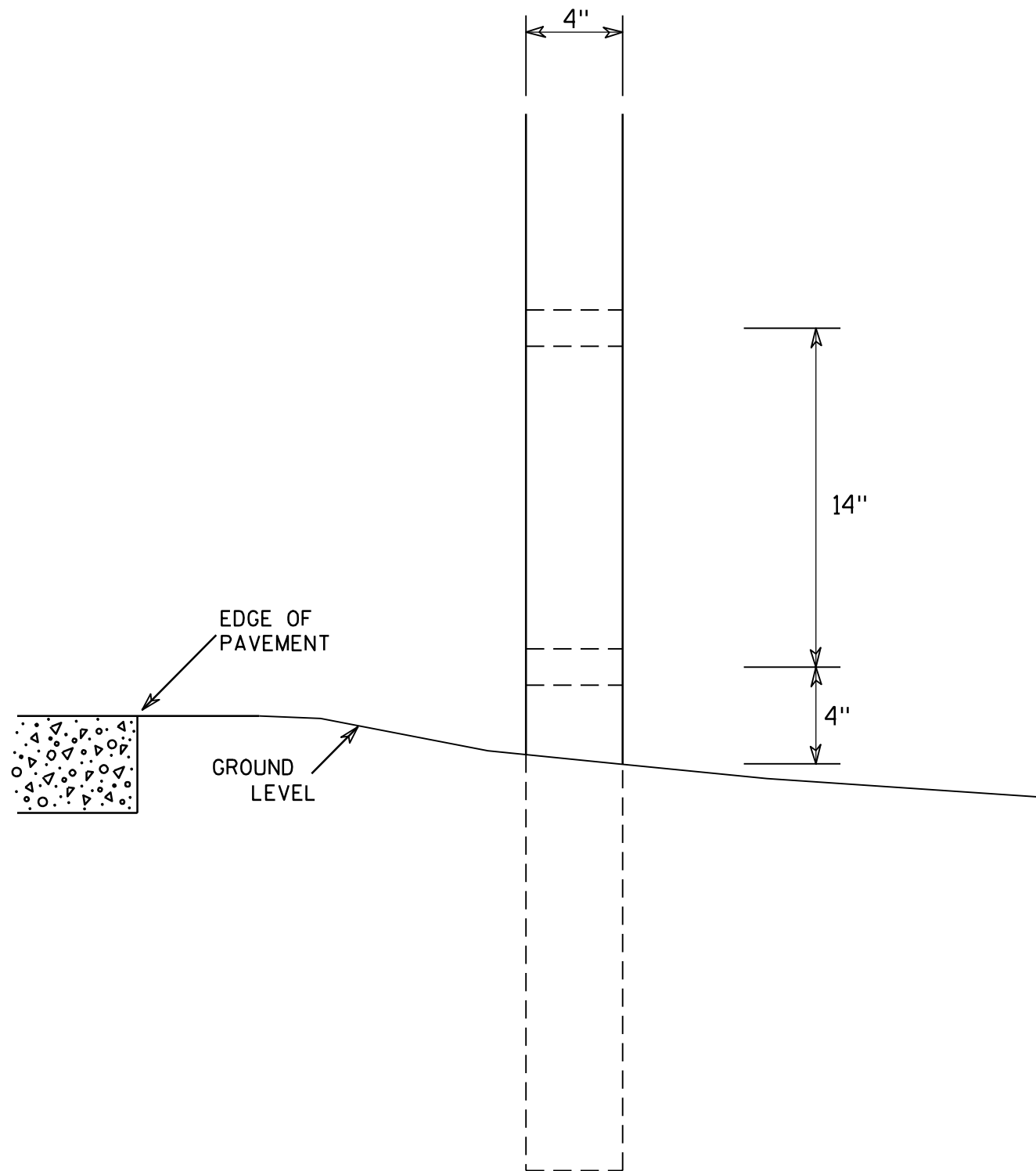
Signs wider than 3 feet or larger than 9 sq. ft shall be mounted on multiple posts (see above table).

**TUBULAR STEEL
SIGN POST
A4-9**

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 2/05/15 PLATE NO. A4-9.9



GENERAL NOTES

1. All 4 x 6 Wood Posts shall be modified by having two 1½" diameter holes drilled perpendicular to the roadway centerline.

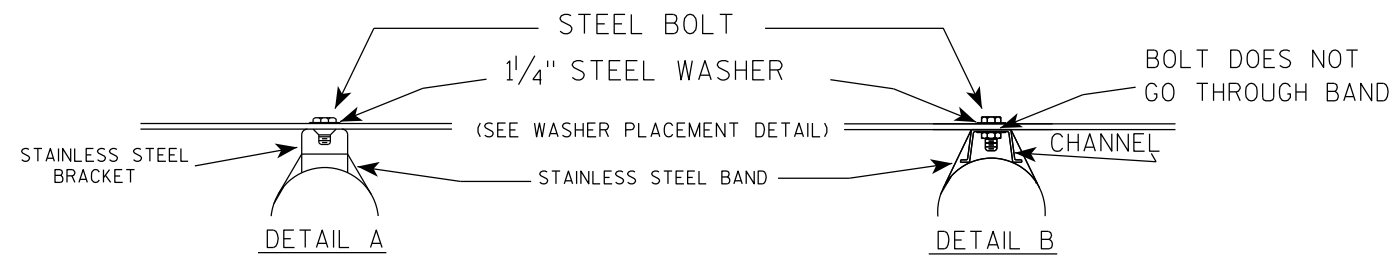
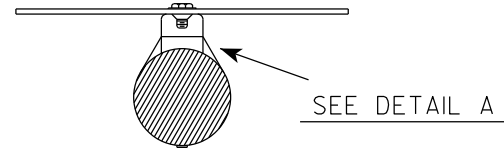
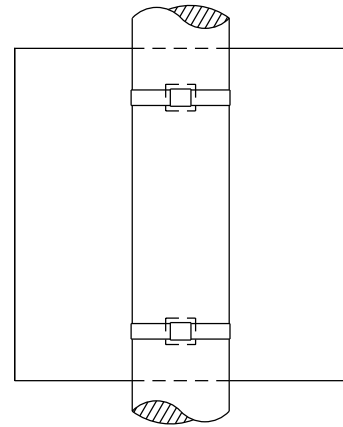
7

7

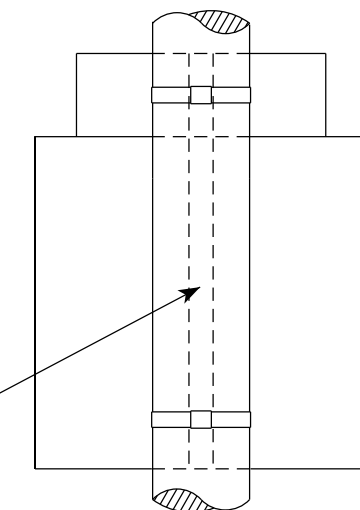
| | |
|------------------------------------------|------------------------------------------------------|
| 4 X 6 WOOD POST MODIFICATIONS | |
| <i>WISCONSIN DEPT OF TRANSPORTATION</i> | |
| APPROVED | <i>Chester J Spang</i> for State Traffic Engineer |
| DATE <u>3/27/97</u> | PLATE NO. <u>A4-11.2</u> |

BANDING

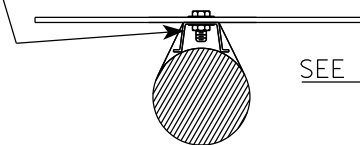
SINGLE SIGN



"J" ASSEMBLY

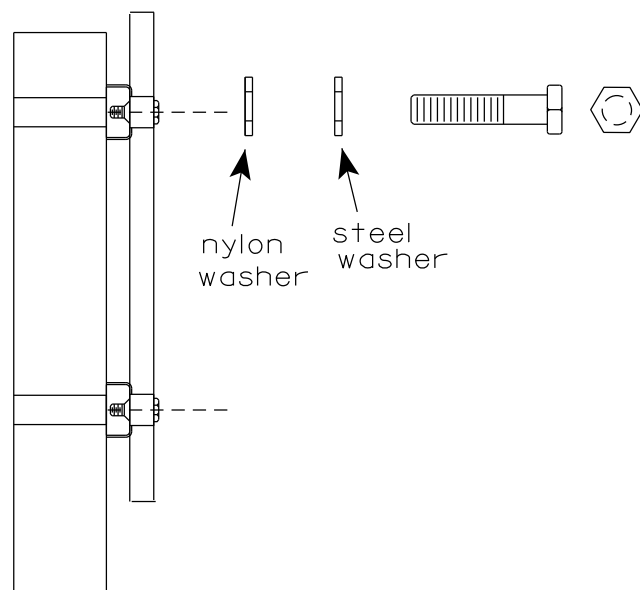


CHANNEL
SEE TYPICAL PANEL
INSTALLATION SHEET



- GENERAL NOTES**
1. Any sign over 3 feet in width shall use the V-Block banding method. See A5-10 standard plate.
 2. Signs 3 feet or greater in height shall have three bracket bands installed. Signs less than 3 feet in height shall have two bracket bands installed.
 3. Banding and assembly bracket shall be stainless steel. All bands shall be $\frac{3}{4}$ " in width and 0.025" thickness.
 4. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
 - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
 - b. Electro-galvanized in accordance with ASTM designation: B 633, Type III, SC 3

WASHER PLACEMENT



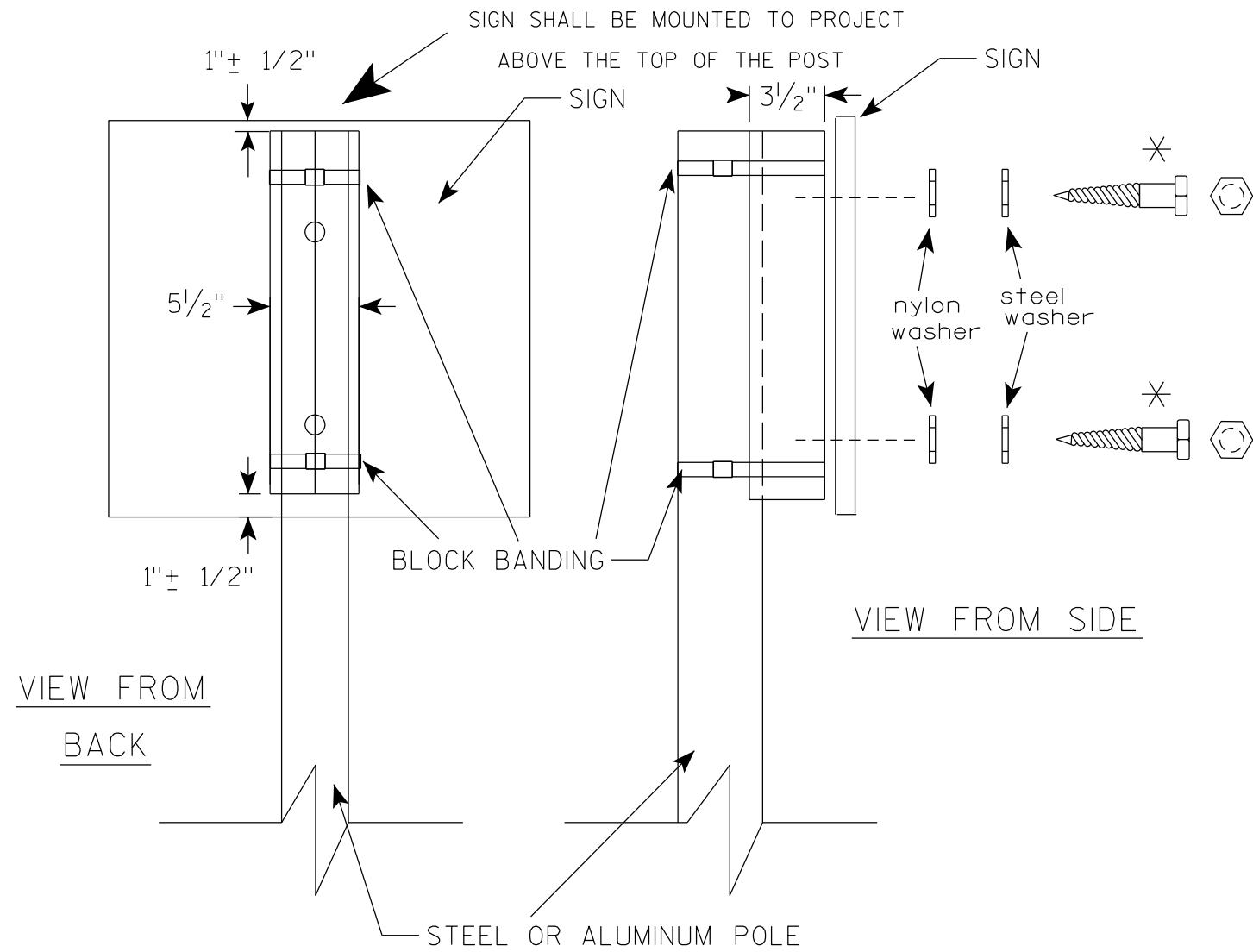
WASHERS (ALL POSTS) -
 1-1/4" O.D. X 3/8" I.D. X 1/16" STEEL
 1-1/4" O.D. X 3/8" I.D. X .080 NYLON
 FOR ALL TYPE H SIGNS

STANDARD SIGN
SIGN BANDING DETAILS

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

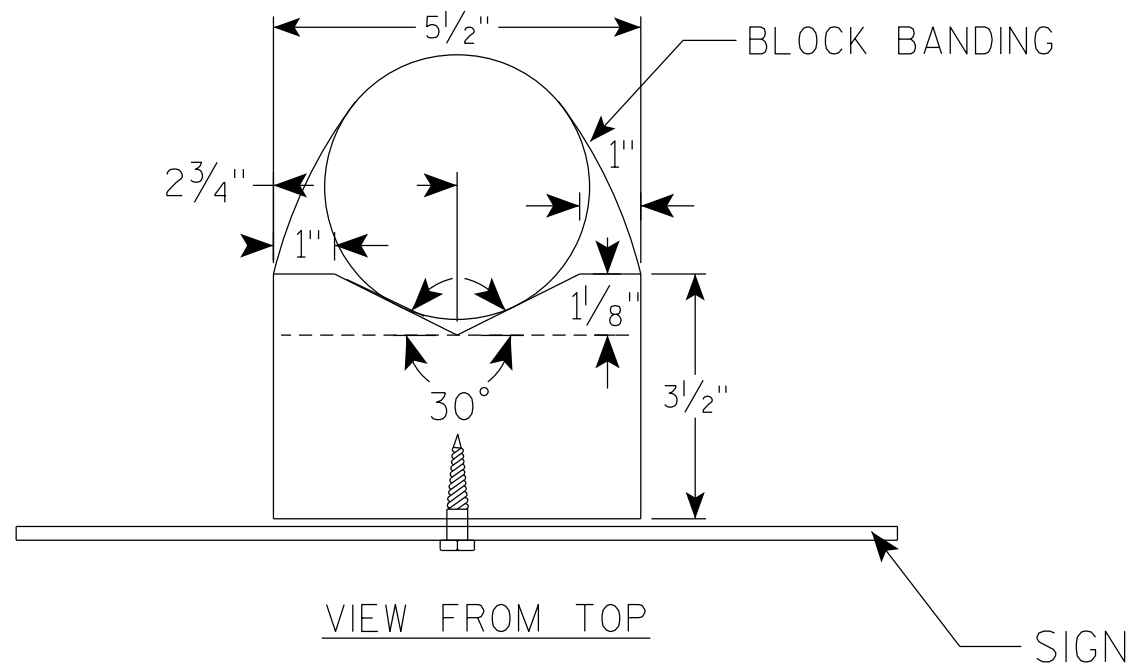
DATE 6/10/19 PLATE NO. A5-9.4



GENERAL NOTES

1. WOOD 4"X6" POST MATERIAL SHALL CONFORM TO 507.2.2 OF THE WISDOT STANDARD SPECIFICATIONS
2. BLOCK BANDING AND CLIPS SHALL BE STAINLESS STEEL, 3/4" WIDTH AND 0.025" THICKNESS
3. SIGNS 3' OR GREATER IN HEIGHT SHALL UTILIZE 3 BLOCK BANDS. SIGNS UNDER 3' IN HEIGHT SHALL UTILIZE 2 BLOCK BANDS
4. ACTUAL NUMBER OF FASTENERS PER SIGN VARIES WITH THE SIGN AREA, BUT NORMALLY THERE ARE TWO. FOR SIGNS GREATER THAN 9 S.F. 3 FASTENERS SHALL BE USED.
5. ALL SIGN MOUNTING BOLTS AND WASHERS SHALL BE EITHER:
 - a. Hot dip or mechanically galvanized in accordance with ASTM Designation: A 153, Class D
 - b. Electro-galvanized in accordance with ASTM Designation : B 633, TYPE III, SC 3
6. ALL BOLTS SHALL HAVE HEXAGONAL HEADS.
7. STEEL WASHERS SHALL BE 1/4" O.D. X 3/8" I.D. X 1/16"
8. NYLON WASHERS SHALL BE 1/4" O.D. X 3/8" I.D. X .080 FOR TYPE H OR TYPE F FACE SIGN

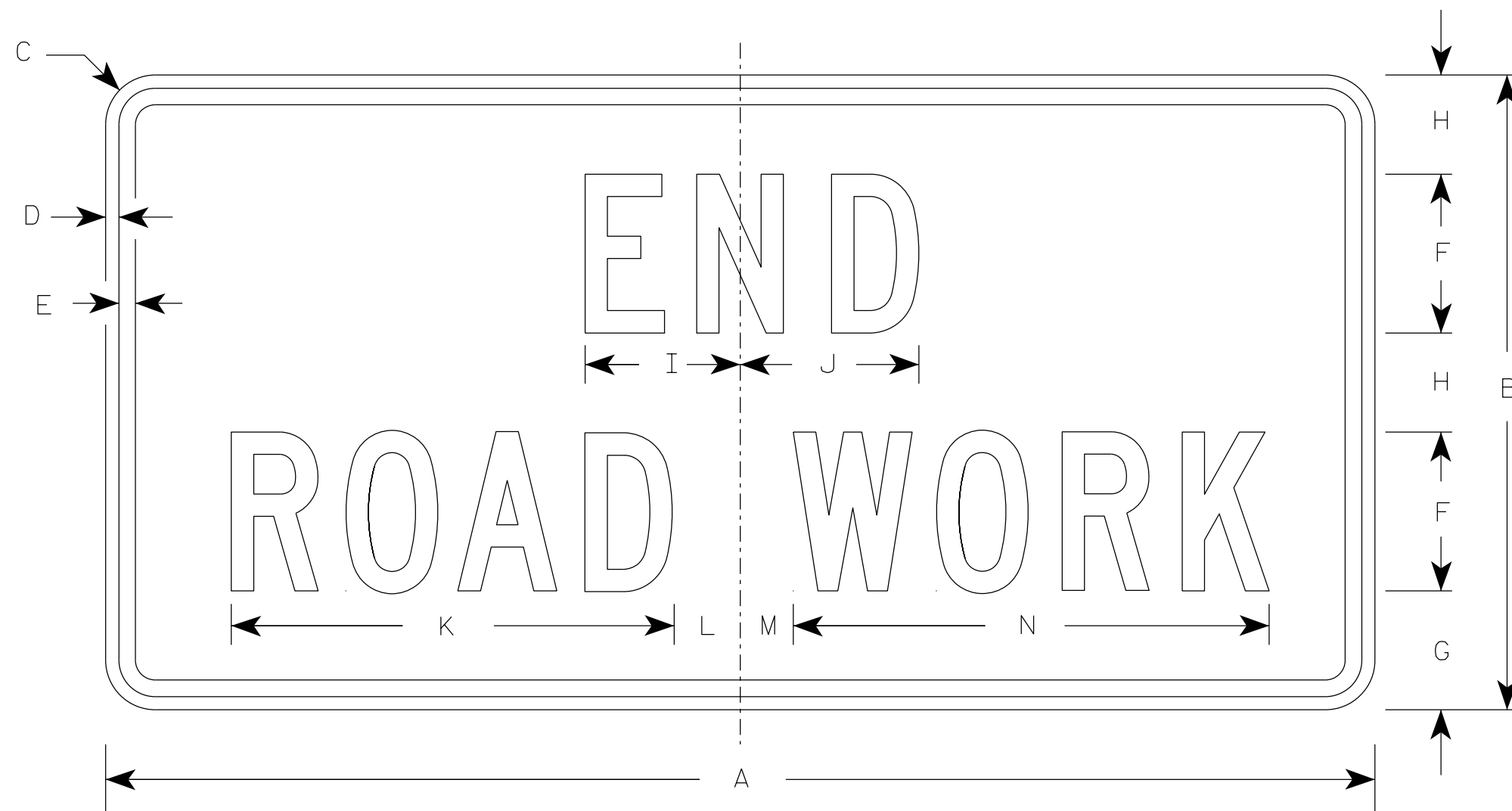
✱ LAG BOLTS SHALL BE 3/8" X 2 1/2"



| | |
|--------------------------------------------|------------------------------------------------------|
| BLOCK BANDING DETAIL (V-BLOCK OPTION) | |
| WISCONSIN DEPT OF TRANSPORTATION | |
| APPROVED | <i>Matthew R Rauch</i> for State Traffic Engineer |
| DATE 4/19/2022 | PLATE NO. A5-10.3 |

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



G20-2A

| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|----|-------|-----|-----|---|-------|-------|-------|-------|--------|-------|-------|--------|---|---|---|---|---|---|---|---|---|---|---|---|-----------------|
| 1 | 36 | 18 | 1 1/2 | 3/8 | 1/2 | 4 | 3 3/4 | 2 1/2 | 4 1/8 | 4 1/8 | 11 1/8 | 2 | 1 | 12 1/8 | | | | | | | | | | | | | 4.5 |
| 2 | 48 | 24 | 1 7/8 | 1/2 | 5/8 | 6 | 4 1/2 | 3 3/4 | 5 7/8 | 6 3/4 | 16 3/4 | 2 1/2 | 1 3/4 | 18 1/2 | | | | | | | | | | | | | 8.0 |
| 2M | 48 | 24 | 1 7/8 | 1/2 | 5/8 | 6 | 4 1/2 | 3 3/4 | 5 7/8 | 6 3/4 | 16 3/4 | 2 1/2 | 1 3/4 | 18 1/2 | | | | | | | | | | | | | 8.0 |
| 3 | 48 | 24 | 1 7/8 | 1/2 | 5/8 | 6 | 4 1/2 | 3 3/4 | 5 7/8 | 6 3/4 | 16 3/4 | 2 1/2 | 1 3/4 | 18 1/2 | | | | | | | | | | | | | 8.0 |
| 4 | 48 | 24 | 1 7/8 | 1/2 | 5/8 | 6 | 4 1/2 | 3 3/4 | 5 7/8 | 6 3/4 | 16 3/4 | 2 1/2 | 1 3/4 | 18 1/2 | | | | | | | | | | | | | 8.0 |
| 5 | 48 | 24 | 1 7/8 | 1/2 | 5/8 | 6 | 4 1/2 | 3 3/4 | 5 7/8 | 6 3/4 | 16 3/4 | 2 1/2 | 1 3/4 | 18 1/2 | | | | | | | | | | | | | 8.0 |

STANDARD SIGN
G20-2A

WISCONSIN DEPT OF TRANSPORTATION

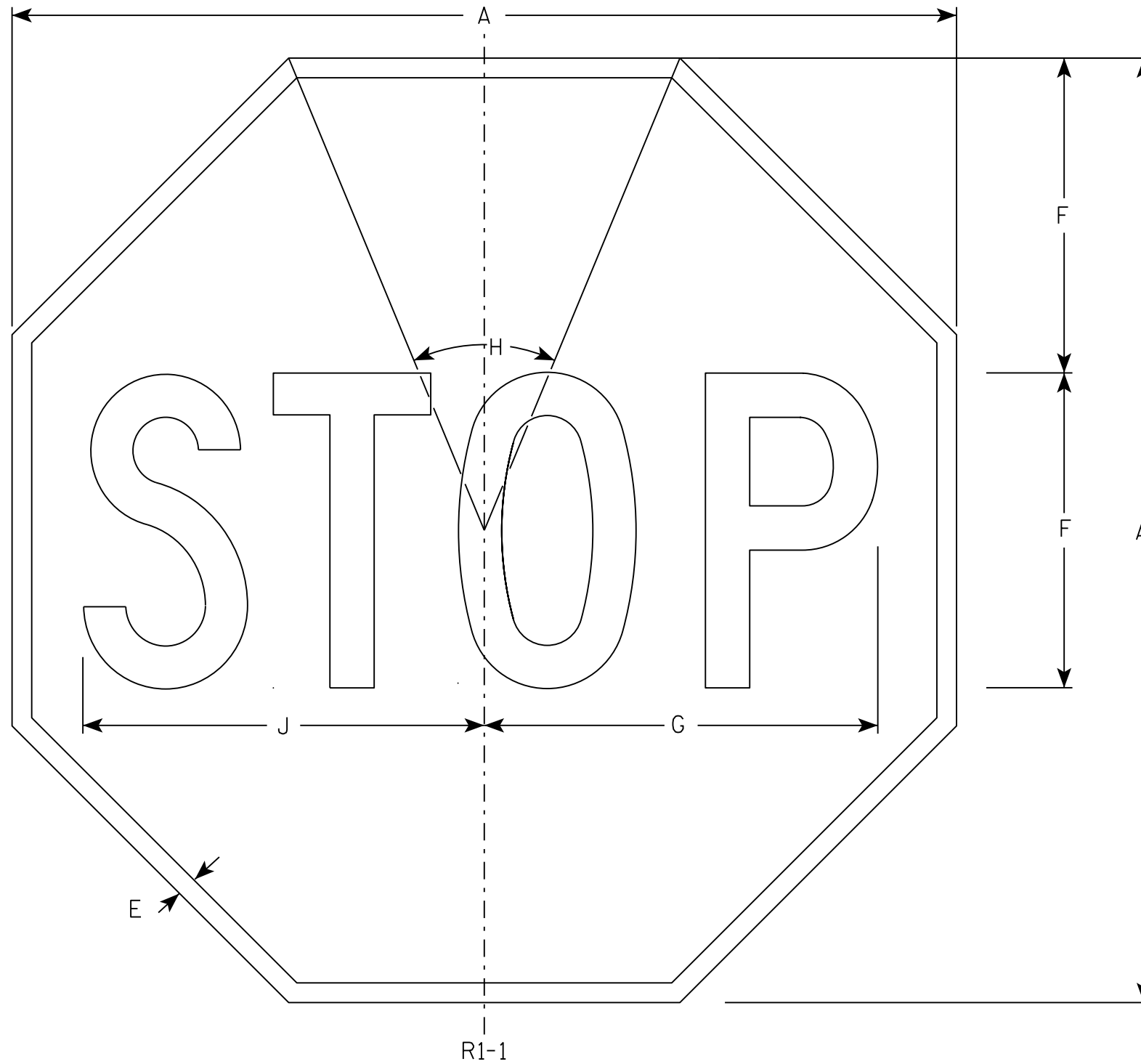
APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 1/26/2023 PLATE NO. G20-2A.10

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**

NOTES

1. Sign is Type II - Type H Reflective - reference WIS DOT Standard Specification for HIGHWAY and STRUCTURE CONSTRUCTION latest edition.
2. Color:
Background - Red
Message - White
3. Message Series - C



7

7

| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|---|---|---|-----|----|--------|-----|---|--------|---|---|---|---|---|---|---|---|---|---|---|---|---|---|---|---|-----------------|
| 1 | 30 | | | | 5/8 | 10 | 12 1/2 | 45° | | 12 3/4 | | | | | | | | | | | | | | | | | 5.18 |
| 2S | 30 | | | | 5/8 | 10 | 12 1/2 | 45° | | 12 3/4 | | | | | | | | | | | | | | | | | 5.18 |
| 2M | 36 | | | | 3/4 | 12 | 15 | 45° | | 15 3/8 | | | | | | | | | | | | | | | | | 7.46 |
| 3 | 36 | | | | 3/4 | 12 | 15 | 45° | | 15 3/8 | | | | | | | | | | | | | | | | | 7.46 |
| 4 | 48 | | | | 1 | 16 | 20 | 45° | | 20 1/2 | | | | | | | | | | | | | | | | | 13.25 |
| 5 | 48 | | | | 1 | 16 | 20 | 45° | | 20 1/2 | | | | | | | | | | | | | | | | | 13.25 |
| 6 | 18 | | | | 3/8 | 6 | 7 3/4 | 45° | | 7 3/4 | | | | | | | | | | | | | | | | | 1.86 |
| 7 | 12 | | | | 1/4 | 4 | 5 | 45° | | 5 1/8 | | | | | | | | | | | | | | | | | 0.78 |

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ **E**

STANDARD SIGN
R1-1

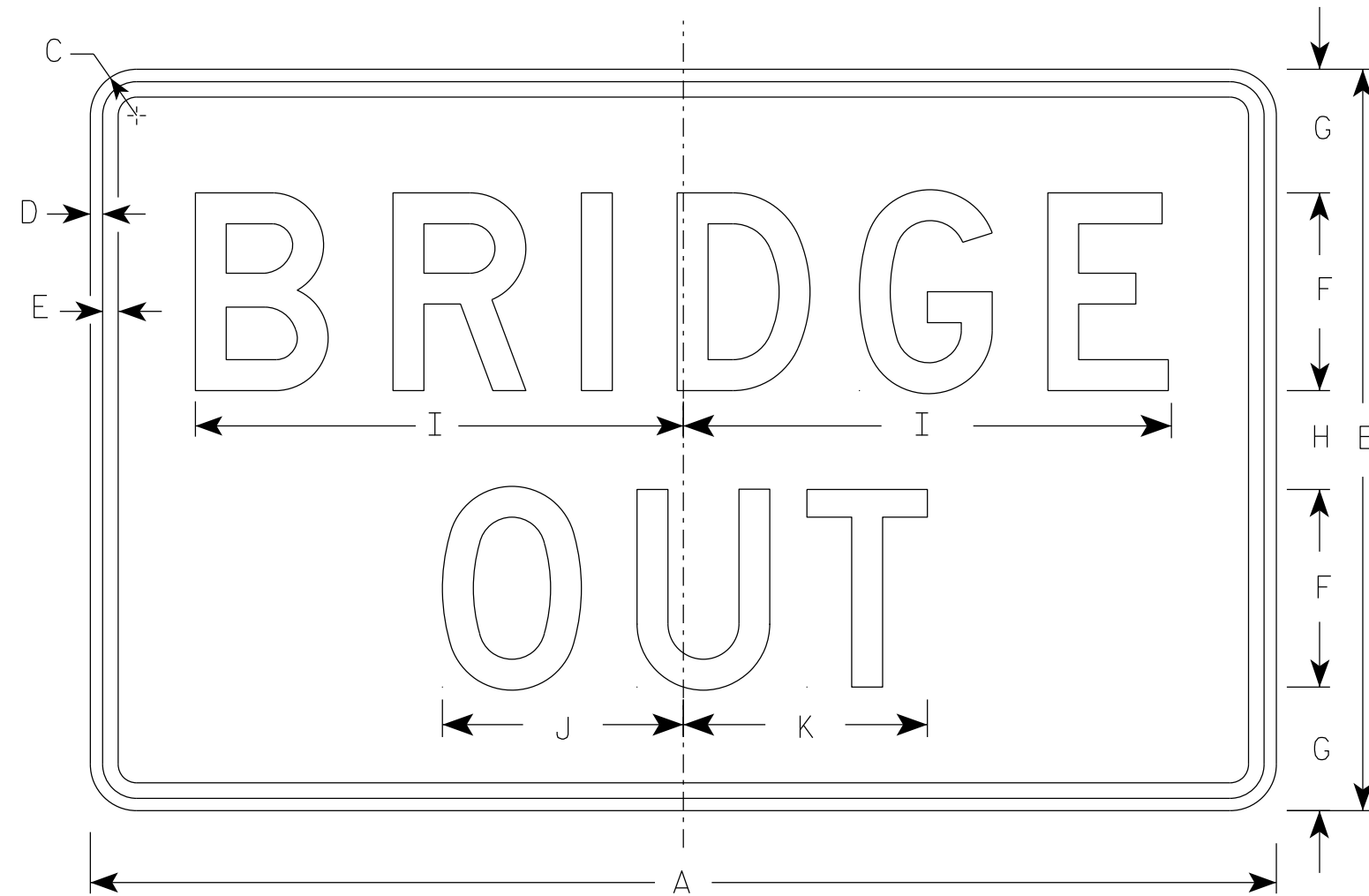
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 11/12/15 PLATE NO. R1-1.13

NOTES

1. Sign is Type II - Type H Reflective
2. Color:
Background - White
Message - Black
3. Message Series - D
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



R11-2B

7

7

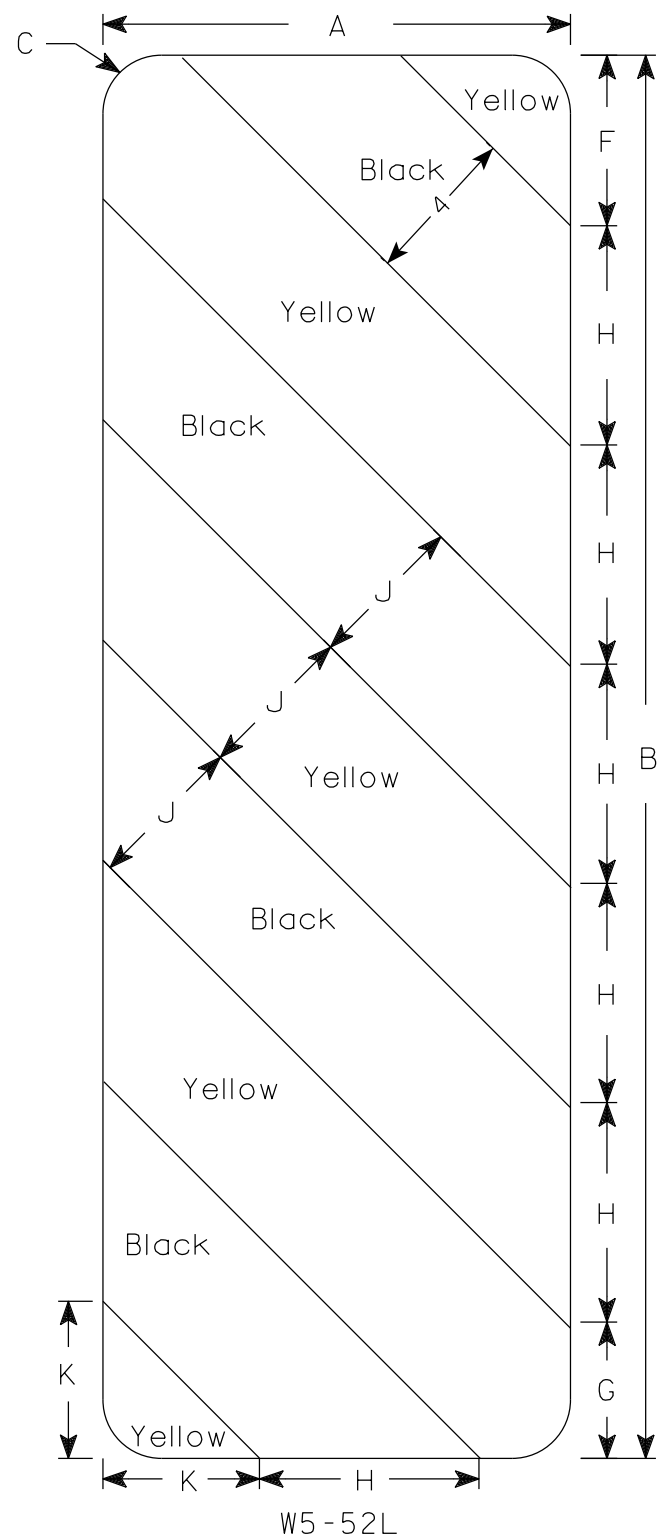
| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|----|-------|-----|-----|---|---|---|--------|-------|-------|---|---|---|---|---|---|---|---|---|---|---|---|---|---|---|-----------------|
| 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2S | 48 | 30 | 1 7/8 | 1/2 | 5/8 | 8 | 5 | 4 | 19 3/4 | 9 3/4 | 9 7/8 | | | | | | | | | | | | | | | | 10.0 |
| 2M | 48 | 30 | 1 7/8 | 1/2 | 5/8 | 8 | 5 | 4 | 19 3/4 | 9 3/4 | 9 7/8 | | | | | | | | | | | | | | | | 10.0 |
| 3 | 48 | 30 | 1 7/8 | 1/2 | 5/8 | 8 | 5 | 4 | 19 3/4 | 9 3/4 | 9 7/8 | | | | | | | | | | | | | | | | 10.0 |
| 4 | 48 | 30 | 1 7/8 | 1/2 | 5/8 | 8 | 5 | 4 | 19 3/4 | 9 3/4 | 9 7/8 | | | | | | | | | | | | | | | | 10.0 |
| 5 | 48 | 30 | 1 7/8 | 1/2 | 5/8 | 8 | 5 | 4 | 19 3/4 | 9 3/4 | 9 7/8 | | | | | | | | | | | | | | | | 10.0 |

STANDARD SIGN
R11-2B

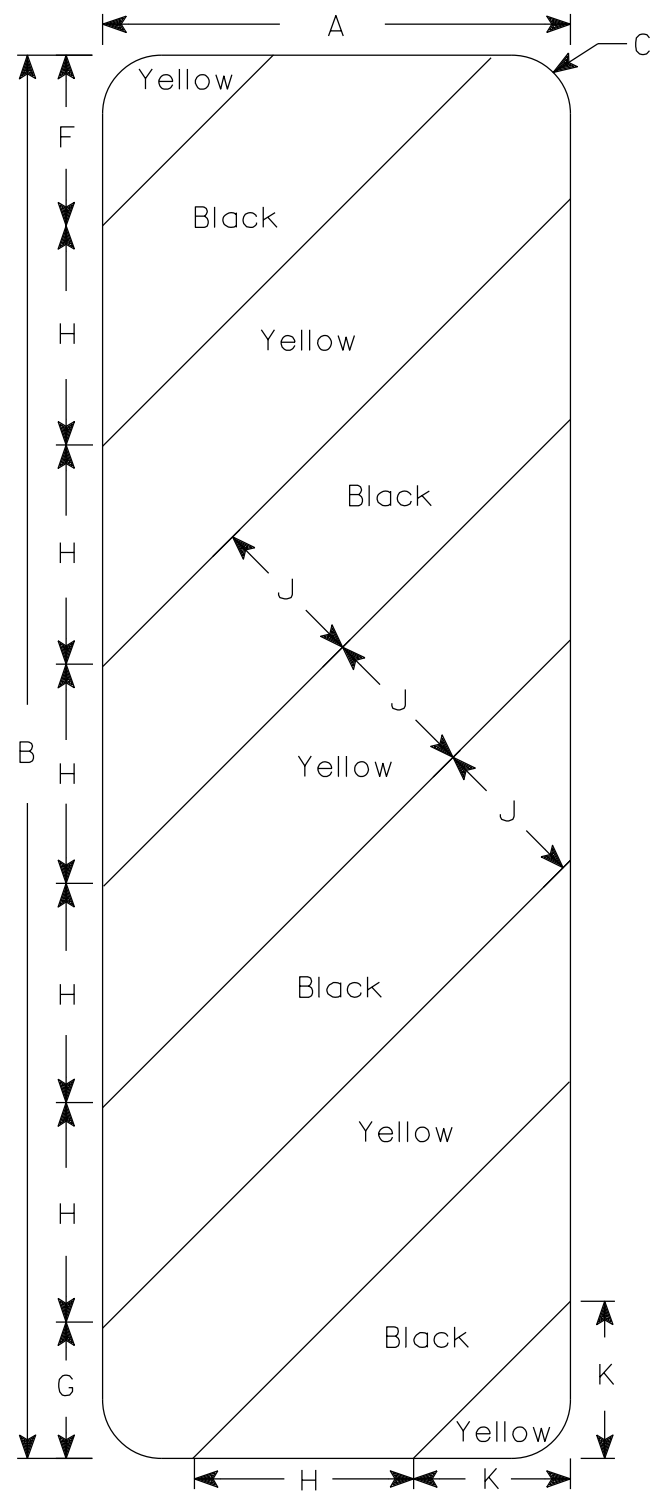
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 2/5/24 PLATE NO. R11-2B.3



W5-52L



W5-52R

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Yellow
Message - Black
3. Alternate colors of stripes as shown.

7

7

| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|----|-------|---|---|-------|-------|-------|-----|---|--------|---|---|---|---|---|---|---|---|---|---|---|---|---|---|---|-----------------|
| 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2S | 12 | 36 | 1 1/2 | | | 4 3/8 | 3 1/2 | 5 5/8 | 45° | 4 | 4 | | | | | | | | | | | | | | | | 3.0 |
| 2M | 12 | 36 | 1 1/2 | | | 4 3/8 | 3 1/2 | 5 5/8 | 45° | 4 | 4 | | | | | | | | | | | | | | | | 3.0 |
| 3 | 18 | 54 | 1 1/2 | | | 6 | 5 1/2 | 8 1/2 | 45° | 6 | 6 9/16 | | | | | | | | | | | | | | | | 6.75 |
| 4 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 5 | | | | | | | | | | | | | | | | | | | | | | | | | | | |

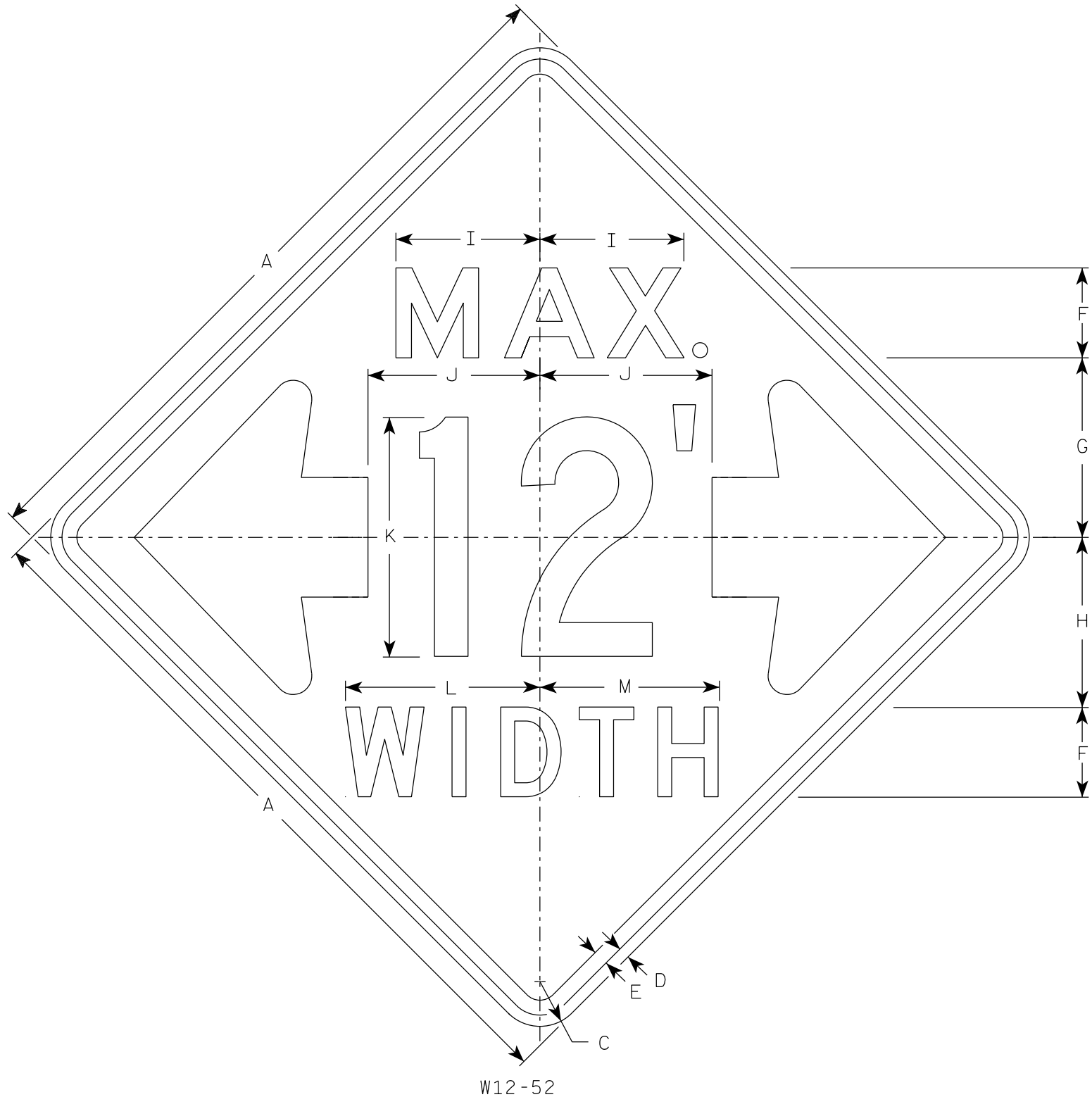
STANDARD SIGN
W5-52L & W5-52R

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 3/4/2024 PLATE NO. W5-52.10

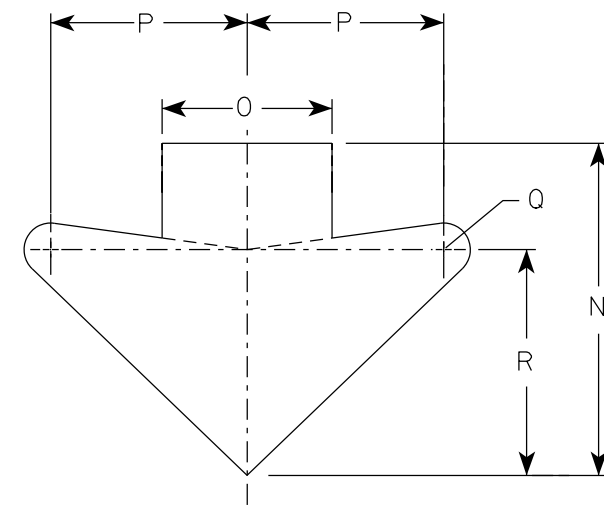
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ **E**



W12-52

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - See note 5
4. The top line is series E, the numerals are series C, and the bottom line is series D.
5. Substitute appropriate numerals and adjust spacing as required.



ARROW DETAIL

7

7

| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|---|---|-----|---|---|----|--------|-------|--------|----|----|----|--------|---|-------|-------|--------|---|---|---|---|---|---|---|---|-----------------|
| 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2S | 48 | | 3 | 3/4 | 1 | 6 | 12 | 11 3/8 | 9 5/8 | 11 1/2 | 16 | 13 | 12 | 15 5/8 | 8 | 9 1/4 | 1 1/4 | 10 5/8 | | | | | | | | | 16.0 |
| 2M | 48 | | 3 | 3/4 | 1 | 6 | 12 | 11 3/8 | 9 5/8 | 11 1/2 | 16 | 13 | 12 | 15 5/8 | 8 | 9 1/4 | 1 1/4 | 10 5/8 | | | | | | | | | 16.0 |
| 3 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 4 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 5 | | | | | | | | | | | | | | | | | | | | | | | | | | | |

STANDARD SIGN
W12-52

WISCONSIN DEPT OF TRANSPORTATION

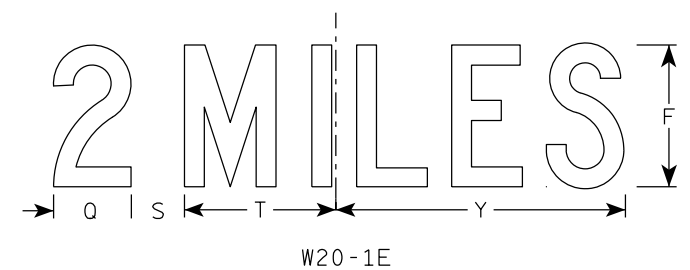
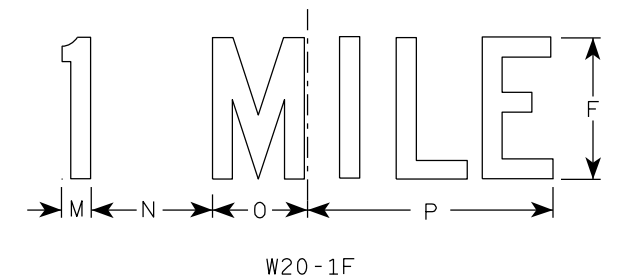
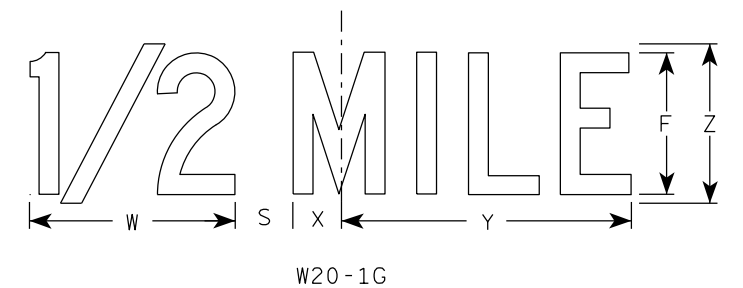
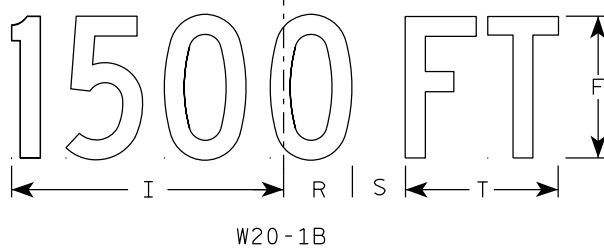
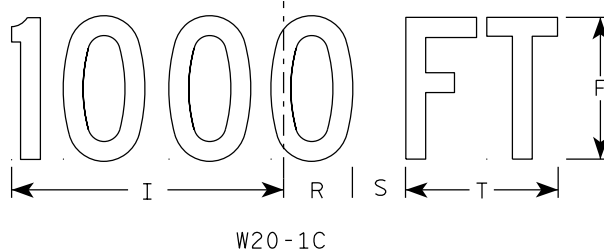
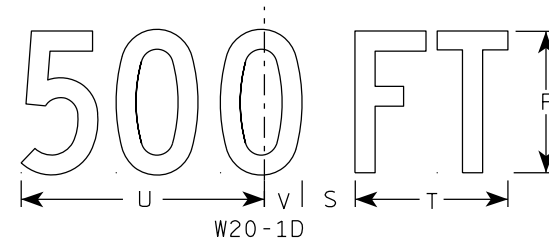
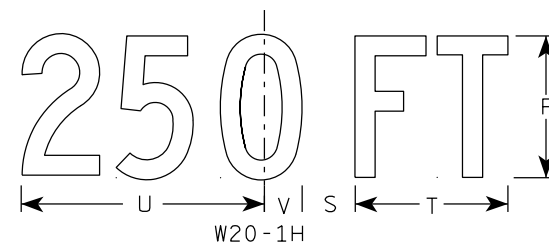
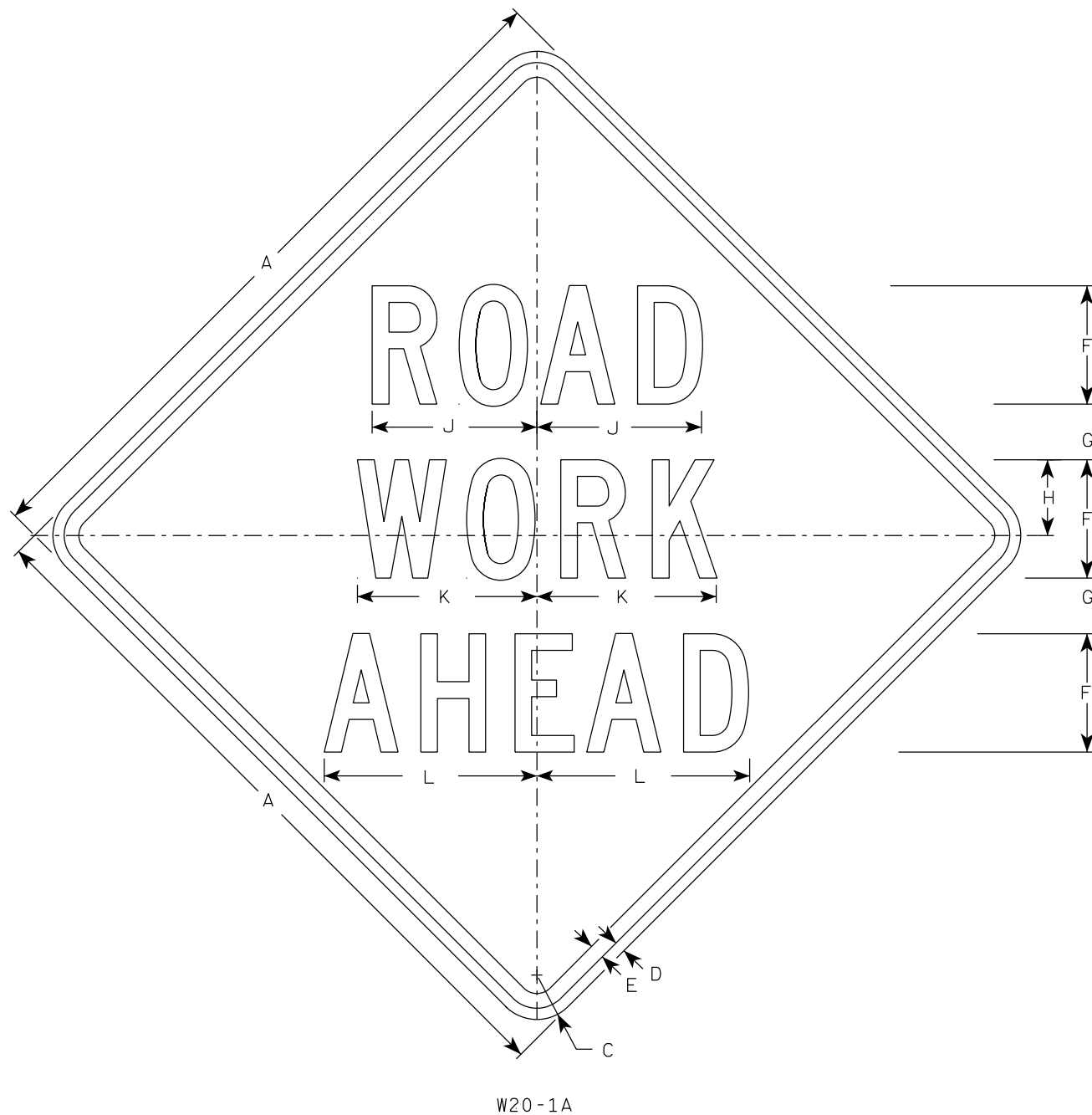
APPROVED *Matthew R. Raub*
For State Traffic Engineer

DATE 3/10/2024 PLATE NO. W12-52.8

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ **E**

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



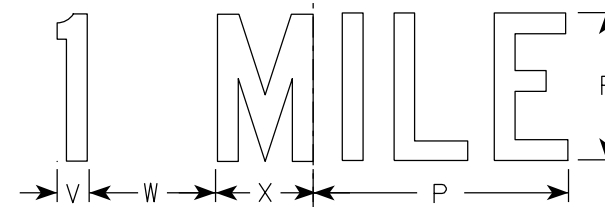
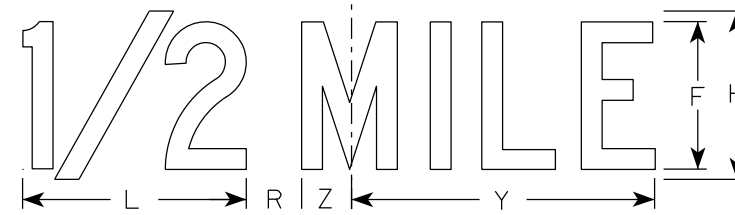
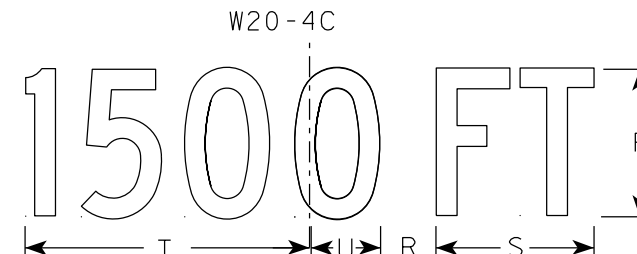
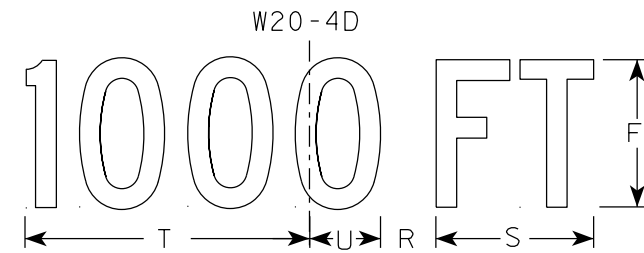
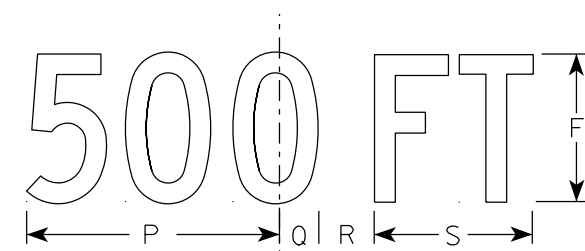
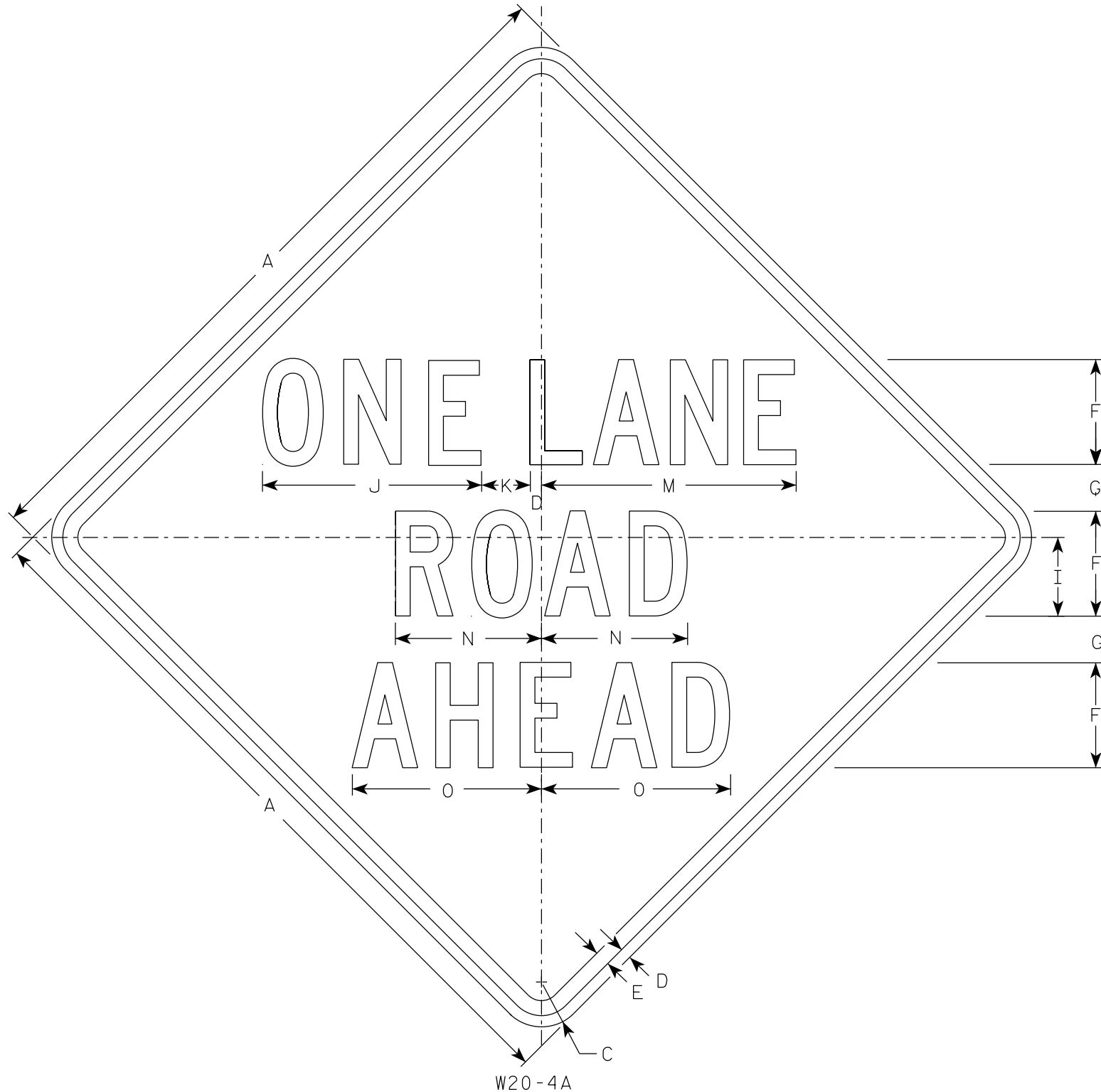
| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|---|-------|-----|-----|---|-------|-------|--------|--------|--------|--------|-------|-------|-------|--------|-------|-------|-------|-------|--------|-------|--------|-------|--------|---|--------------|
| 1 | 36 | | 2 1/4 | 5/8 | 3/4 | 5 | 2 5/8 | 3 1/4 | 10 1/8 | 7 | 7 5/8 | 8 7/8 | 1 1/8 | 4 1/2 | 3 1/2 | 9 | 3 1/4 | 2 1/2 | 2 1/4 | 5 5/8 | 9 | 1 3/8 | 8 | 1 3/4 | 10 3/4 | 6 | 9.0 |
| 2S | 48 | | 3 | 3/4 | 1 | 8 | 3 3/4 | 5 1/8 | 15 3/8 | 11 1/8 | 12 1/8 | 14 3/8 | 1 5/8 | 6 7/8 | 5 3/8 | 13 7/8 | 4 3/8 | 3 7/8 | 3 | 8 5/8 | 13 3/4 | 2 1/8 | 11 7/8 | 2 3/4 | 16 3/8 | 9 | 16.0 |
| 2M | 48 | | 3 | 3/4 | 1 | 8 | 3 3/4 | 5 1/8 | 15 3/8 | 11 1/8 | 12 1/8 | 14 3/8 | 1 5/8 | 6 7/8 | 5 3/8 | 13 7/8 | 4 3/8 | 3 7/8 | 3 | 8 5/8 | 13 3/4 | 2 1/8 | 11 7/8 | 2 3/4 | 16 3/8 | 9 | 16.0 |
| 3 | 48 | | 3 | 3/4 | 1 | 8 | 3 3/4 | 5 1/8 | 15 3/8 | 11 1/8 | 12 1/8 | 14 3/8 | 1 5/8 | 6 7/8 | 5 3/8 | 13 7/8 | 4 3/8 | 3 7/8 | 3 | 8 5/8 | 13 3/4 | 2 1/8 | 11 7/8 | 2 3/4 | 16 3/8 | 9 | 16.0 |
| 4 | 48 | | 3 | 3/4 | 1 | 8 | 3 3/4 | 5 1/8 | 15 3/8 | 11 1/8 | 12 1/8 | 14 3/8 | 1 5/8 | 6 7/8 | 5 3/8 | 13 7/8 | 4 3/8 | 3 7/8 | 3 | 8 5/8 | 13 3/4 | 2 1/8 | 11 7/8 | 2 3/4 | 16 3/8 | 9 | 16.0 |
| 5 | 48 | | 3 | 3/4 | 1 | 8 | 3 3/4 | 5 1/8 | 15 3/8 | 11 1/8 | 12 1/8 | 14 3/8 | 1 5/8 | 6 7/8 | 5 3/8 | 13 7/8 | 4 3/8 | 3 7/8 | 3 | 8 5/8 | 13 3/4 | 2 1/8 | 11 7/8 | 2 3/4 | 16 3/8 | 9 | 16.0 |

STANDARD SIGN
W20-1A, B, C, D, E, F, G & H

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
For State Traffic Engineer

DATE 1/10/2024 PLATE NO. W20-1.12



NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Message Series - C
4. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.

7

7

| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|---|-------|-----|-----|---|-------|---|-------|--------|-------|--------|--------|-------|--------|----|-------|-------|-------|--------|-------|-------|-------|-------|--------|-------|-----------------|
| 1 | 36 | | 2 1/4 | 5/8 | 3/4 | 5 | 2 3/8 | 6 | 3 3/4 | 10 3/8 | 2 3/8 | 8 | 13 1/2 | 7 | 8 7/8 | 9 | 1 3/8 | 1 7/8 | 5 5/8 | 10 1/8 | 2 1/2 | 1 1/8 | 4 1/2 | 3 1/2 | 10 3/4 | 1 3/4 | 9.0 |
| 2S | 48 | | 3 | 3/4 | 1 | 7 | 3 1/8 | 8 | 5 1/4 | 14 5/8 | 3 1/4 | 10 5/8 | 17 3/4 | 9 3/4 | 12 5/8 | 12 | 1 7/8 | 2 5/8 | 7 1/2 | 13 1/2 | 3 3/8 | 1 1/2 | 6 | 4 5/8 | 14 3/8 | 2 3/8 | 16.0 |
| 2M | 48 | | 3 | 3/4 | 1 | 7 | 3 1/8 | 8 | 5 1/4 | 14 5/8 | 3 1/4 | 10 5/8 | 17 3/4 | 9 3/4 | 12 5/8 | 12 | 1 7/8 | 2 5/8 | 7 1/2 | 13 1/2 | 3 3/8 | 1 1/2 | 6 | 4 5/8 | 14 3/8 | 2 3/8 | 16.0 |
| 3 | 48 | | 3 | 3/4 | 1 | 7 | 3 1/8 | 8 | 5 1/4 | 14 5/8 | 3 1/4 | 10 5/8 | 17 3/4 | 9 3/4 | 12 5/8 | 12 | 1 7/8 | 2 5/8 | 7 1/2 | 13 1/2 | 3 3/8 | 1 1/2 | 6 | 4 5/8 | 14 3/8 | 2 3/8 | 16.0 |
| 4 | 48 | | 3 | 3/4 | 1 | 7 | 3 1/8 | 8 | 5 1/4 | 14 5/8 | 3 1/4 | 10 5/8 | 17 3/4 | 9 3/4 | 12 5/8 | 12 | 1 7/8 | 2 5/8 | 7 1/2 | 13 1/2 | 3 3/8 | 1 1/2 | 6 | 4 5/8 | 14 3/8 | 2 3/8 | 16.0 |
| 5 | 48 | | 3 | 3/4 | 1 | 7 | 3 1/8 | 8 | 5 1/4 | 14 5/8 | 3 1/4 | 10 5/8 | 17 3/4 | 9 3/4 | 12 5/8 | 12 | 1 7/8 | 2 5/8 | 7 1/2 | 13 1/2 | 3 3/8 | 1 1/2 | 6 | 4 5/8 | 14 3/8 | 2 3/8 | 16.0 |

STANDARD SIGN
W20-4A, B, C, D, F & G

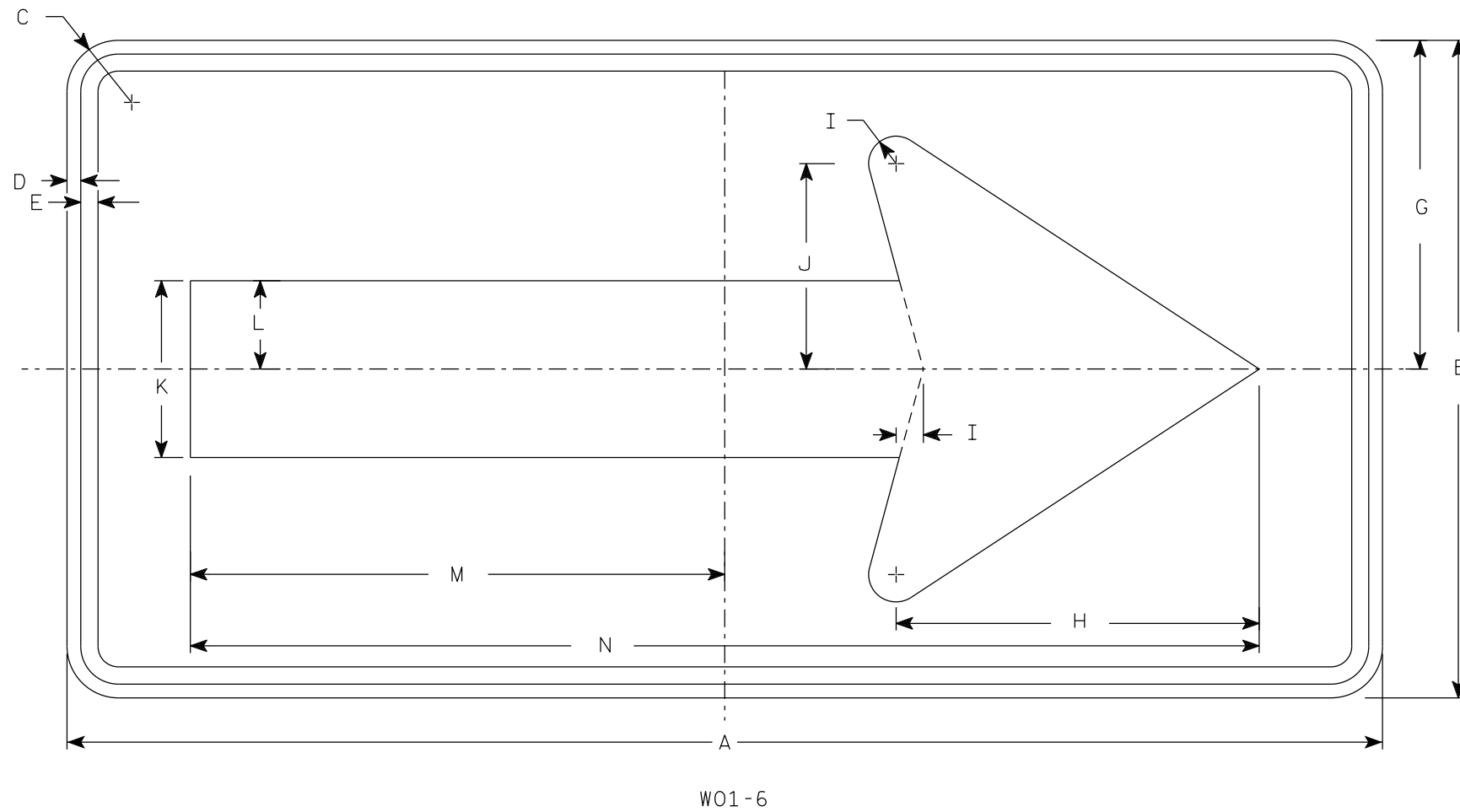
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 1/10/2024 PLATE NO. W20-4.10

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
3. Corners may be square or rounded when base material is plywood but borders shall be rounded as shown. When base material is metal, the corners and borders shall be rounded.



7

7

| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|----|-------|-----|-----|---|----|--------|-------|-------|-------|-------|--------|--------|---|---|---|---|---|---|---|---|---|---|---|---|-----------------|
| 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2S | 48 | 24 | 1 7/8 | 1/2 | 5/8 | | 12 | 13 1/4 | 1 | 7 1/2 | 6 1/2 | 3 1/4 | 19 1/2 | 39 | | | | | | | | | | | | | 8.0 |
| 2M | 48 | 24 | 1 7/8 | 1/2 | 5/8 | | 12 | 13 1/4 | 1 | 7 1/2 | 6 1/2 | 3 1/4 | 19 1/2 | 39 | | | | | | | | | | | | | 8.0 |
| 3 | 60 | 30 | 1 7/8 | 1/2 | 5/8 | | 15 | 16 1/4 | 1 1/4 | 9 1/4 | 8 | 4 | 24 3/8 | 48 3/4 | | | | | | | | | | | | | 12.5 |
| 4 | 60 | 30 | 1 7/8 | 1/2 | 5/8 | | 15 | 16 1/4 | 1 1/4 | 9 1/4 | 8 | 4 | 24 3/8 | 48 3/4 | | | | | | | | | | | | | 12.5 |
| 5 | 60 | 30 | 1 7/8 | 1/2 | 5/8 | | 15 | 16 1/4 | 1 1/4 | 9 1/4 | 8 | 4 | 24 3/8 | 48 3/4 | | | | | | | | | | | | | 12.5 |

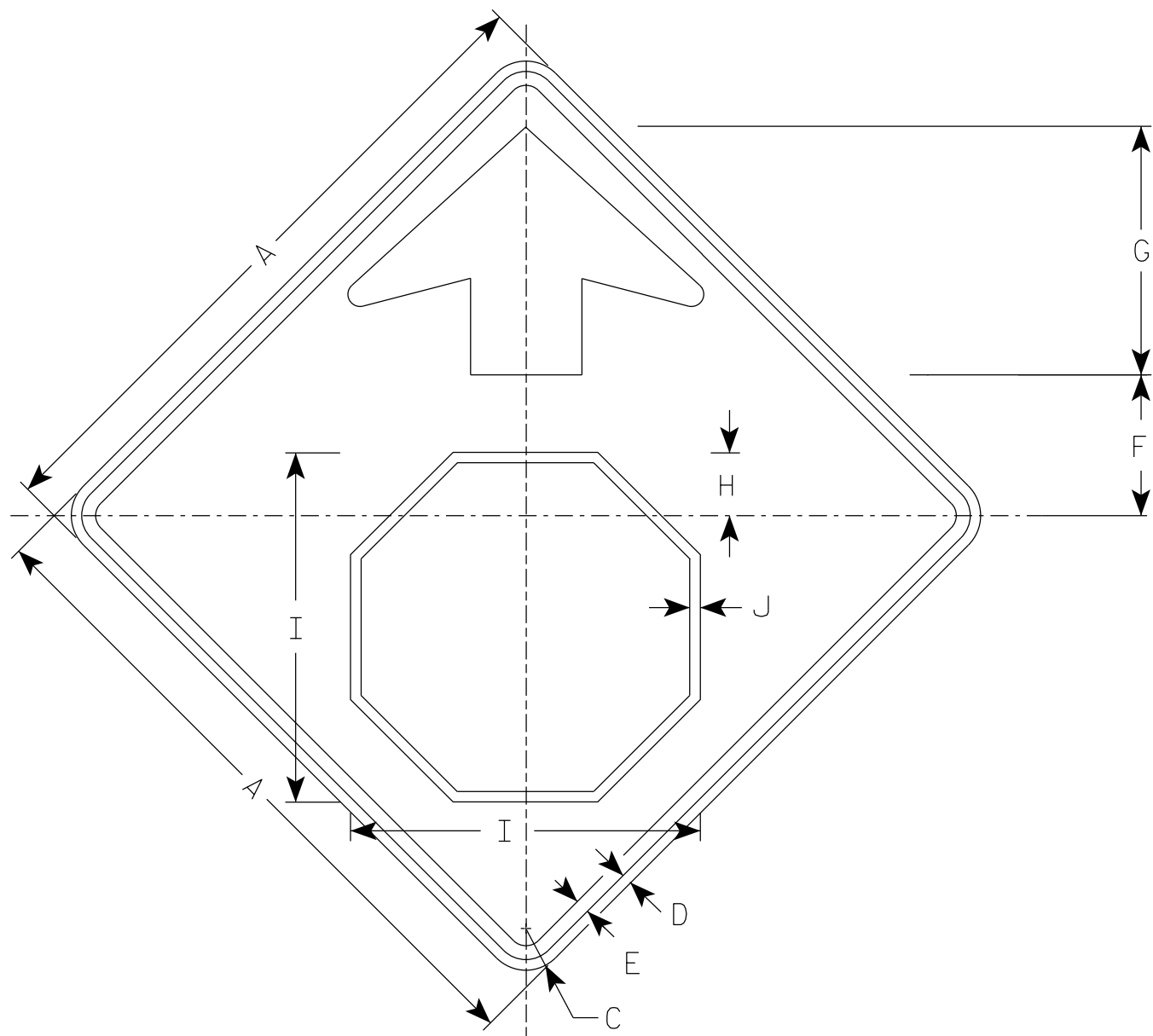
STANDARD SIGN
W01-6

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 1/24/2024 PLATE NO. W01-6.2

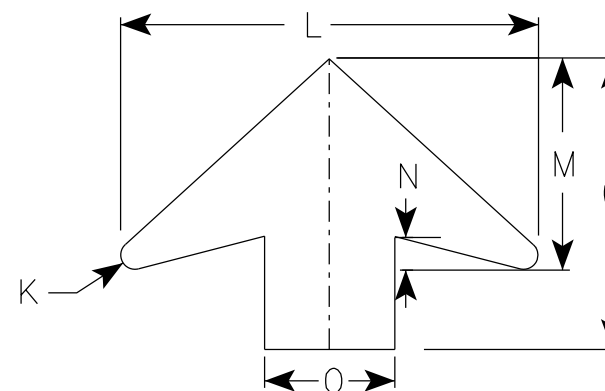
PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: **E**



W03-1

NOTES

1. All Signs Type II - Type F Reflective
2. Color:
 - Background - ORANGE
 - Arrow & Border - BLACK
 - Stop Symbol - WHITE BORDER ON RED BACKGROUND



ARROW DETAIL

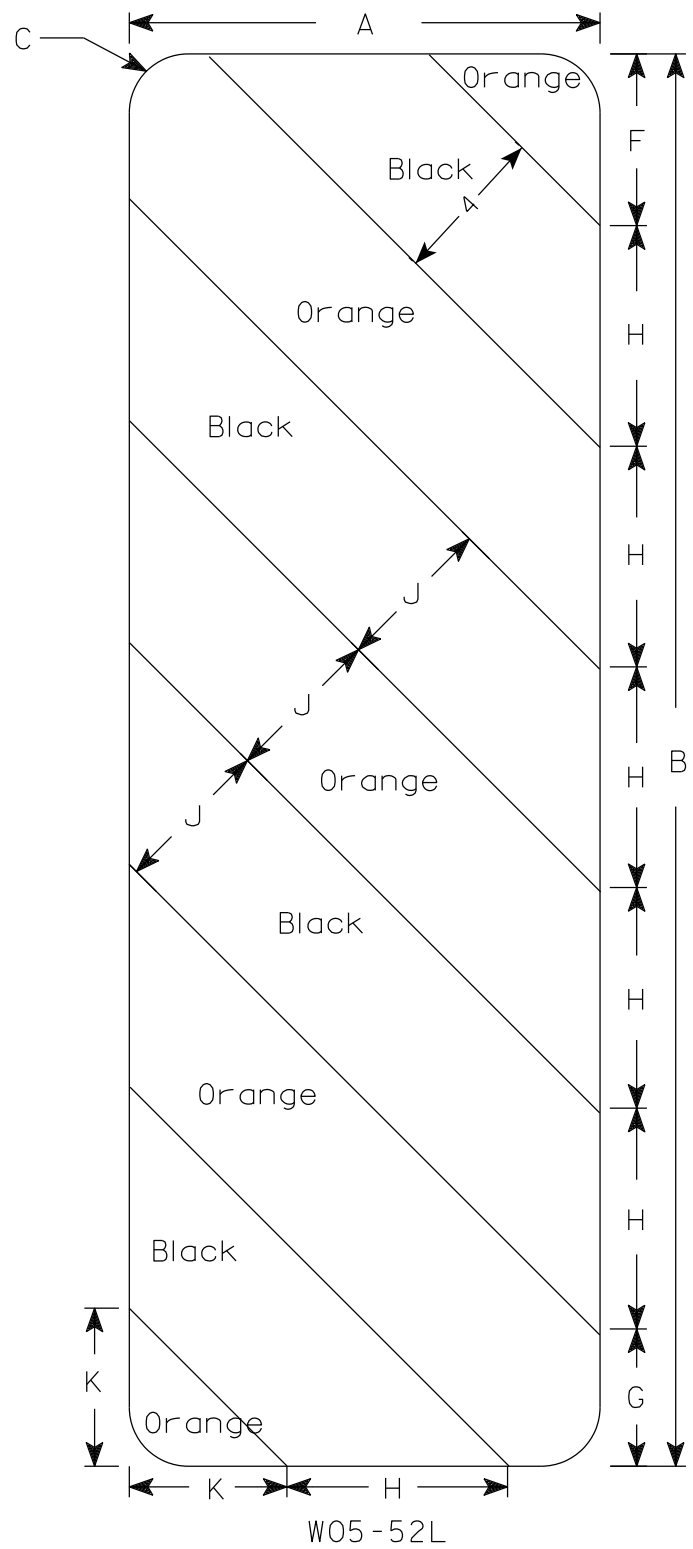
| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|---|-------|-----|-----|-------|--------|-------|--------|-----|-----|--------|-------|-------|---|---|---|---|---|---|---|---|---|---|---|---|-----------------|
| 1 | 36 | | 2 1/4 | 5/8 | 3/4 | 7 1/2 | 13 1/2 | 3 1/2 | 19 | 5/8 | 5/8 | 19 1/4 | 9 3/4 | 1 5/8 | 6 | | | | | | | | | | | | 9.0 |
| 2S | 48 | | 3 | 3/4 | 1 | 10 | 17 7/8 | 4 1/2 | 25 1/8 | 3/4 | 7/8 | 25 5/8 | 13 | 2 | 8 | | | | | | | | | | | | 16.0 |
| 2M | 48 | | 3 | 3/4 | 1 | 10 | 17 7/8 | 4 1/2 | 25 1/8 | 3/4 | 7/8 | 25 5/8 | 13 | 2 | 8 | | | | | | | | | | | | 16.0 |
| 3 | 48 | | 3 | 3/4 | 1 | 10 | 17 7/8 | 4 1/2 | 25 1/8 | 3/4 | 7/8 | 25 5/8 | 13 | 2 | 8 | | | | | | | | | | | | 16.0 |
| 4 | 48 | | 3 | 3/4 | 1 | 10 | 17 7/8 | 4 1/2 | 25 1/8 | 3/4 | 7/8 | 25 5/8 | 13 | 2 | 8 | | | | | | | | | | | | 16.0 |
| 5 | 48 | | 3 | 3/4 | 1 | 10 | 17 7/8 | 4 1/2 | 25 1/8 | 3/4 | 7/8 | 25 5/8 | 13 | 2 | 8 | | | | | | | | | | | | 16.0 |

STANDARD SIGN
W03-1

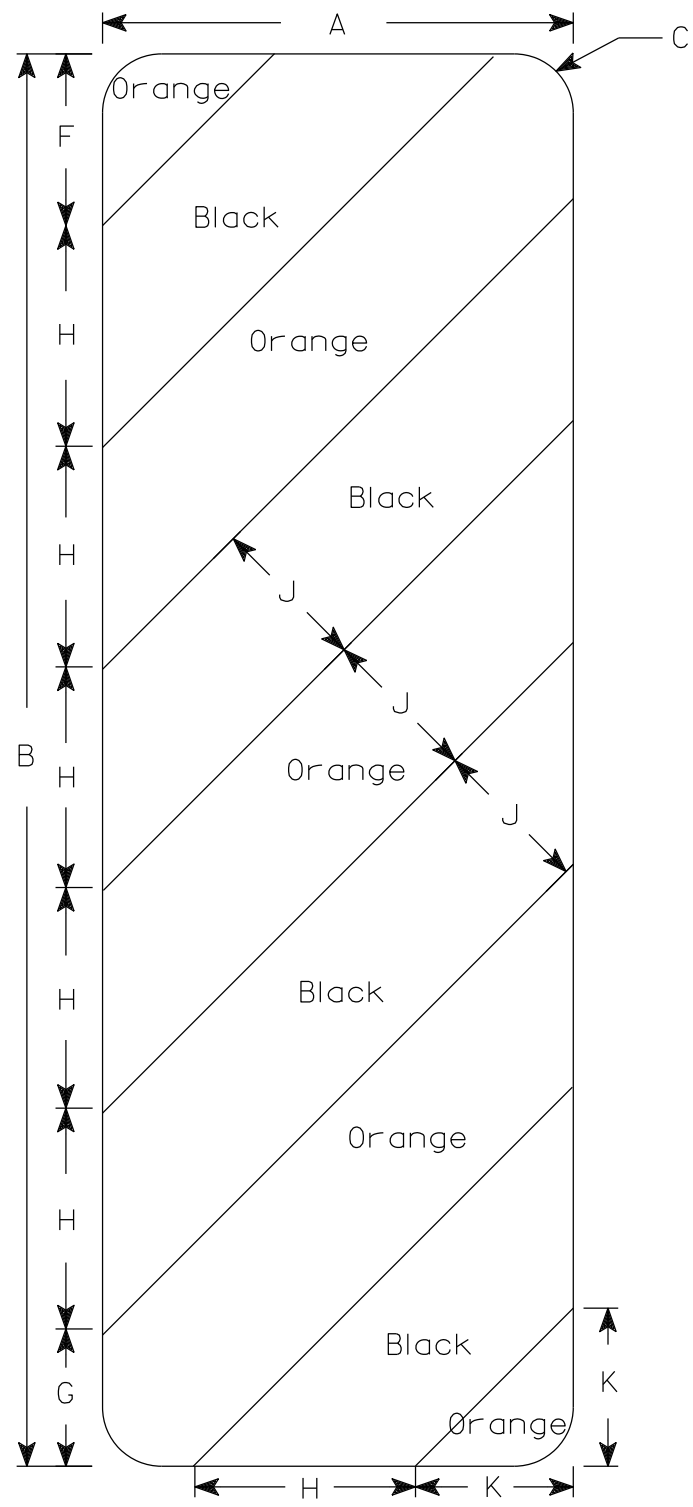
WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 1/24/2024 PLATE NO. W03-1.2



W05-52L



W05-52R

NOTES

1. Sign is Type II - Type F Reflective
2. Color:
Background - Orange
Message - Black
4. Alternate colors of stripes as shown.

7

7

| SIZE | A | B | C | D | E | F | G | H | I | J | K | L | M | N | O | P | Q | R | S | T | U | V | W | X | Y | Z | Area sq. ft. |
|------|----|----|-------|---|---|-------|-------|-------|-----|---|--------|---|---|---|---|---|---|---|---|---|---|---|---|---|---|---|-----------------|
| 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2S | 12 | 36 | 1 1/2 | | | 4 3/8 | 3 1/2 | 5 5/8 | 45° | 4 | 4 | | | | | | | | | | | | | | | | 3.0 |
| 2M | 12 | 36 | 1 1/2 | | | 4 3/8 | 3 1/2 | 5 5/8 | 45° | 4 | 4 | | | | | | | | | | | | | | | | 3.0 |
| 3 | 18 | 54 | 1 1/2 | | | 6 | 5 1/2 | 8 1/2 | 45° | 6 | 6 9/16 | | | | | | | | | | | | | | | | 6.75 |
| 4 | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 5 | | | | | | | | | | | | | | | | | | | | | | | | | | | |

STANDARD SIGN
W05-52L & W05-52R

WISCONSIN DEPT OF TRANSPORTATION

APPROVED *Matthew R. Rauch*
for State Traffic Engineer

DATE 3/13/2024 PLATE NO. W05-52.2

PROJECT NO: _____ HWY: _____ COUNTY: _____ SHEET NO: _____ **E**

DESIGN DATA

LIVE LOAD:

DESIGN LOADING: HL-93
 INVENTORY RATING FACTOR: RF = 1.03
 OPERATING RATING FACTOR: RF = 1.34
 WISCONSIN STANDARD PERMIT VEHICLE (WIS.-SPV): 250 (KIPS)

STRUCTURE IS DESIGNED FOR A FUTURE WEARING SURFACE OF 20 POUNDS PER SQUARE FOOT.

MATERIAL PROPERTIES:

CONCRETE MASONRY: _____ f'c = 4,000 P.S.I.
 SUPERSTRUCTURE _____ f'c = 3,500 P.S.I.
 ALL OTHER _____
 BAR STEEL REINFORCEMENT: _____ fy = 60,000 P.S.I.
 GRADE 60

FOUNDATION DATA

ABUTMENTS TO BE SUPPORTED ON CIP 10 3/4 X 0.25 PILING DRIVEN TO A REQUIRED DRIVING RESISTANCE OF 90 TONS ++ PER PILE AS DETERMINED BY THE MODIFIED GATES DYNAMIC FORMULA. ESTIMATED 65 FEET LONG.

++ THE FACTORED AXIAL RESISTANCE OF PILES IN COMPRESSION USED FOR DESIGN IS THE REQUIRED DRIVING RESISTANCE MULTIPLIED BY A RESISTANCE FACTOR OF 0.5 USING MODIFIED GATES TO DETERMINE DRIVEN PILE CAPACITY.

TRAFFIC VOLUME

FEATURE ON: LACKEY LANE
 ADT = 50 (2044)
 R.D.S. = 25 M.P.H.

HYDRAULIC DATA

100 YEAR FREQUENCY

Q₁₀₀ = 221 C.F.S.
 VEL. = 3.77 F.P.S.
 HW₁₀₀ = EL. 866.37
 WATERWAY AREA = 70 SQ. FT.
 DRAINAGE AREA = 2.02 SQ. MI.
 ROADWAY OVERTOPPING = NA
 SCOUR CRITICAL CODE = 5

2 YEAR FREQUENCY

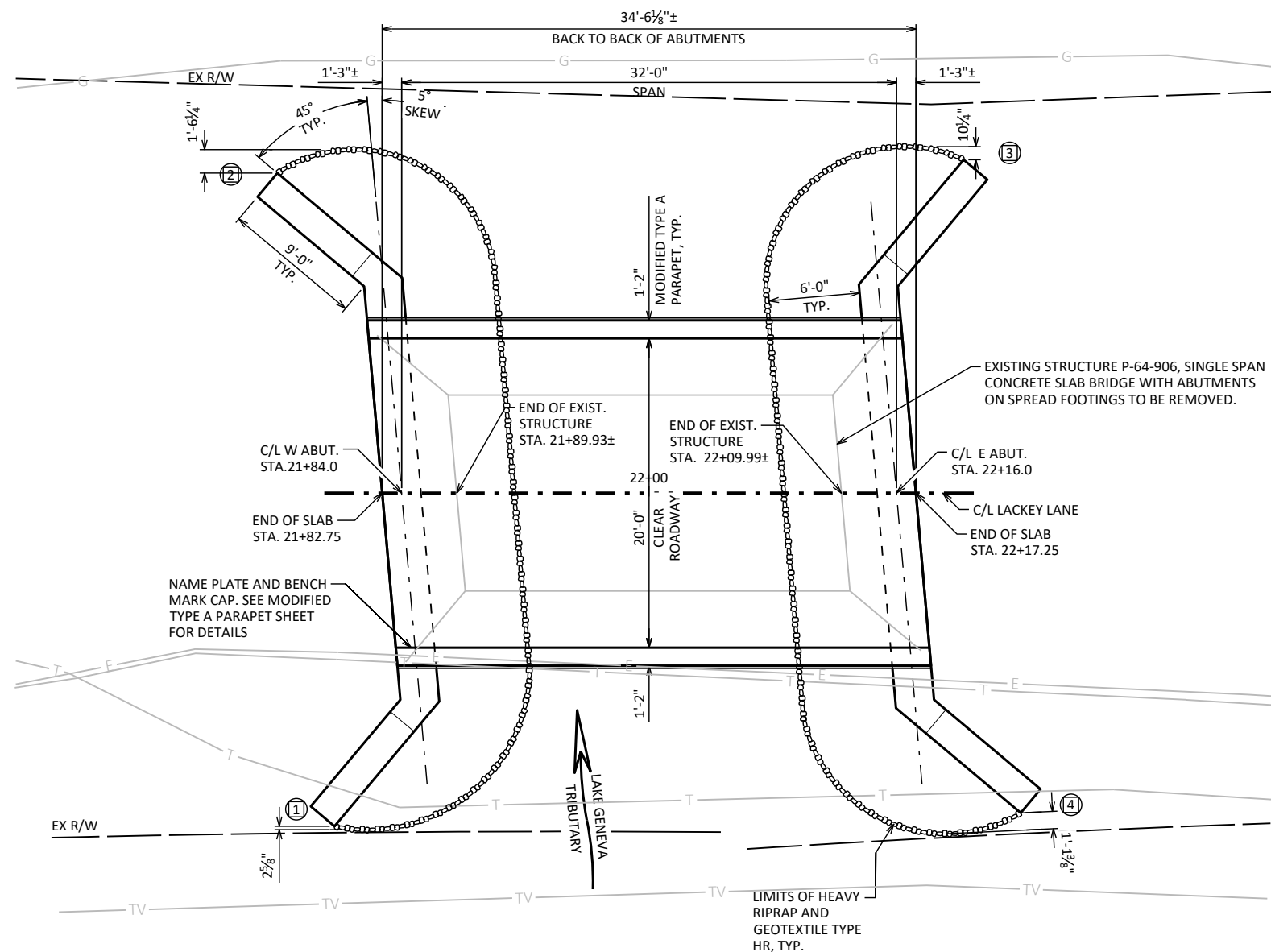
Q₂ = 68 C.F.S.
 VEL. = 2.9 F.P.S.
 HW₂ = EL. 864.7

TEMPORARY STRUCTURE

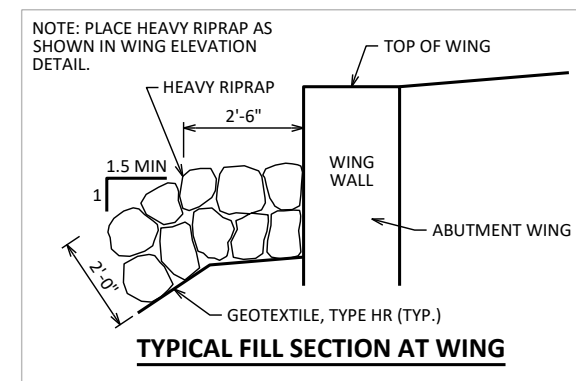
Q₅ = 100 C.F.S.
 HW₅ = 865.8
 MIN. ABR. = 21.5 SQ. FT.

LIST OF DRAWINGS

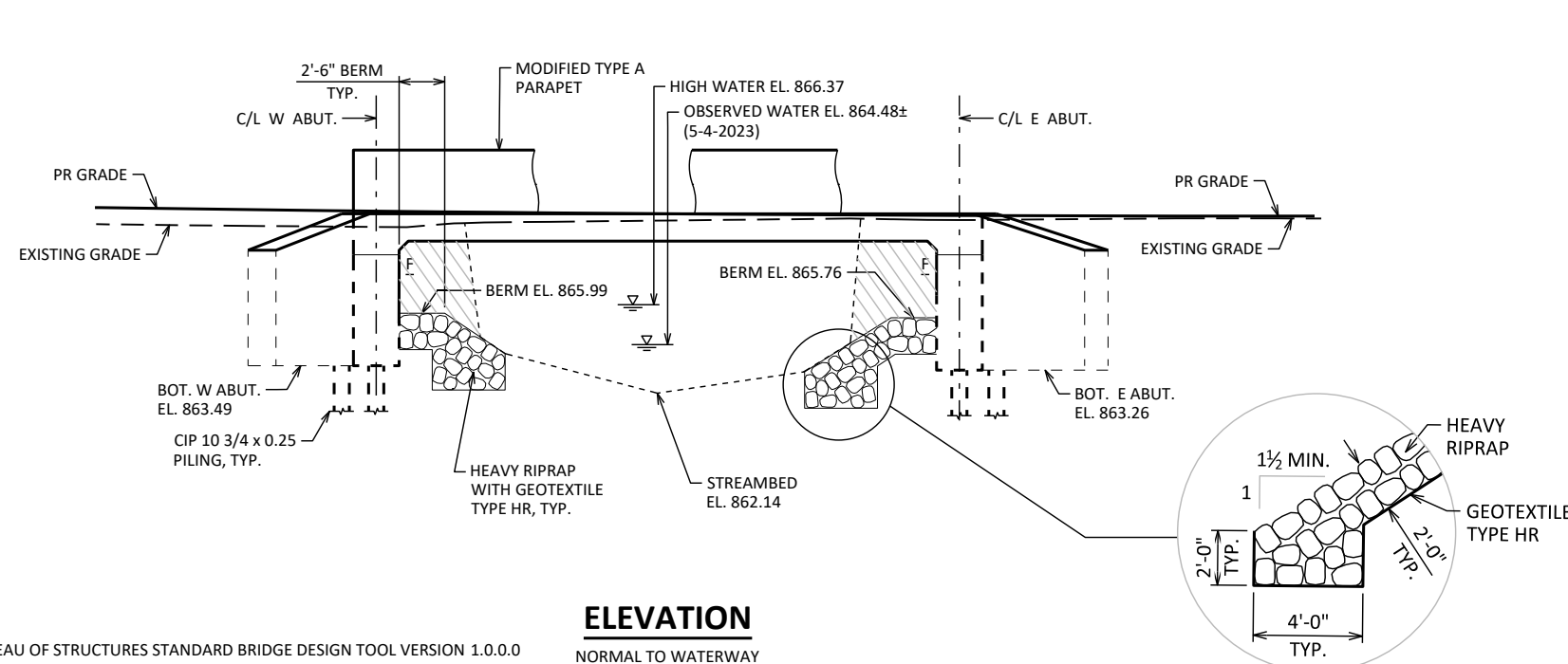
1. GENERAL PLAN
2. CROSS SECTION & QUANTITIES
3. SUBSURFACE EXPLORATION
4. WEST ABUTMENT
5. WEST ABUTMENT DETAILS
6. EAST ABUTMENT
7. EAST ABUTMENT DETAILS
8. SUPERSTRUCTURE
9. SUPERSTRUCTURE DETAILS
10. MODIFIED TYPE A PARAPET DETAILS



PLAN
 SINGLE SPAN FLAT SLAB



TYPICAL FILL SECTION AT WING

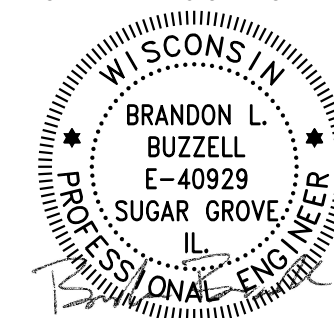


ELEVATION
 NORMAL TO WATERWAY

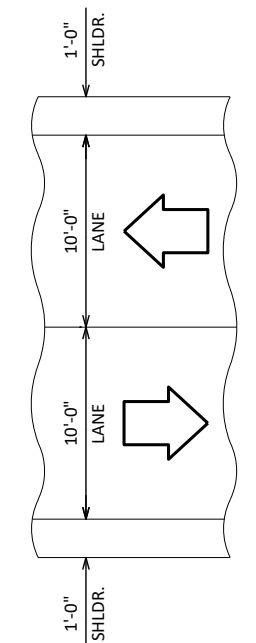
STRUCTURE DESIGN CONTACTS:

BRANDON BUZZELL 815-444-4415
 AARON BONK 608-261-0261

THESE PLANS ARE BASED UPON STANDARD BRIDGE PLANS DEVELOPED AND MAINTAINED BY THE WISCONSIN DEPARTMENT OF TRANSPORTATION THROUGH THE USE OF THE WISDOT STANDARD BRIDGE DESIGN TOOL. THE UNDERSIGNED DESIGNER CERTIFIES THE ACCURACY OF THE BRIDGE TYPE, SIZE AND LOCATION, HYDRAULICS AND FOUNDATION SUPPORT, AND INFORMATION IN THE PLANS THAT IS NOT PART OF THE STANDARD PLANS SUPPLIED BY THE DEPARTMENT. THE DESIGNER FURTHER CERTIFIES THAT USE OF THE STANDARD BRIDGE DESIGN TOOL FOR DEVELOPMENT OF THIS PLAN IS CONSISTENT WITH THE GUIDANCE PROVIDED IN THE WISDOT BRIDGE MANUAL.



| NO. | DATE | REVISION | BY |
|------------------------------------------------------|----------|-----------------------|---------------|
| BAXTER & WOODMAN Consulting Engineers | | | |
| STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION | | | |
| ACCEPTED | SDR | | 04/03/24 |
| CHIEF STRUCTURES DESIGN ENGINEER | | DATE | |
| STRUCTURE B-64-0214 | | | |
| LACKEY LANE OVER LAKE GENEVA TRIBUTARY | | | |
| COUNTY | WALWORTH | TOWN, CITY OR VILLAGE | TOWN OF LINN |
| DESIGN SPEC. AASHTO LRFD BRIDGE DESIGN SPECIFICATION | | | |
| DESIGNED BY | SM | DESIGNED CK'D | BLB |
| DRAWN BY | UKB | PLANS CK'D | BLB |
| GENERAL PLAN | | | SHEET 1 OF 10 |



INDICATES WING NUMBER

8

8

GENERAL NOTES

DRAWINGS SHALL NOT BE SCALED.

BAR STEEL REINFORCEMENT SHALL BE EMBEDDED 2" CLEAR UNLESS OTHERWISE SHOWN OR NOTED.

THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE.

BEVEL EXPOSED EDGES OF CONCRETE 3/4" UNLESS OTHERWISE NOTED.

THE UPPER LIMITS OF "EXCAVATION FOR STRUCTURES B-64-0214" SHALL BE THE EXISTING GROUNDLINE.

AT THE BACK FACE OF ABUTMENT ALL VOLUME WHICH CANNOT BE PLACED BEFORE ABUTMENT CONSTRUCTION AND IS NOT OCCUPIED BY THE NEW STRUCTURE SHALL BE BACKFILLED WITH STRUCTURE BACKFILL TYPE A.

EXCAVATION BELOW THE ABUTMENT AND ABUTMENT BEDDING MATERIALS REQUIRES ENGINEER APPROVAL. GEOTEXTILE SHALL BE SET AT THE BOTTOM OF EXCAVATION AND EXTEND 2'-0" ABOVE BOTTOM OF ABUTMENT.

THE QUANTITY FOR BACKFILL STRUCTURE IS CALCULATED BASED ON THE DETAIL SHOWN IN THE PLANS.

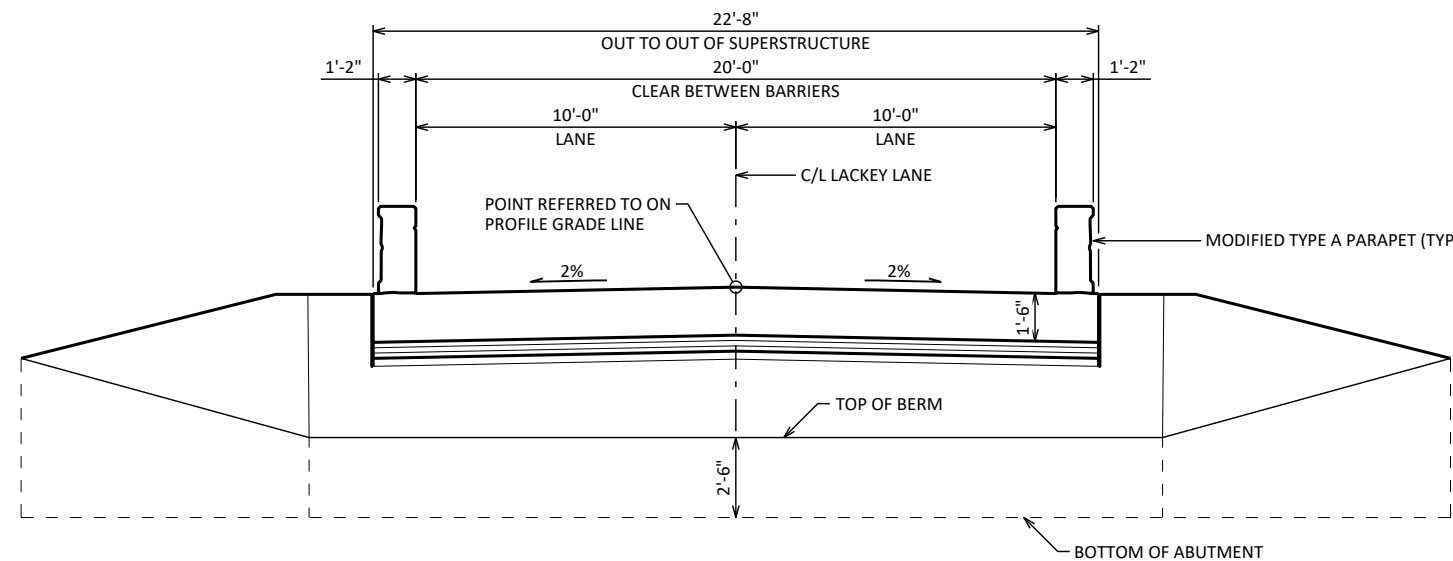
THE SLOPE OF THE FILL IN FRONT OF THE ABUTMENTS SHALL BE COVERED WITH HEAVY RIPRAP AND GEOTEXTILE TYPE HR TO THE EXTENT SHOWN ON SHEET 1 AND THE ABUTMENT DETAILS.

AT ABUTMENTS, CONCRETE POURED UNDER WATER WILL BE ALLOWED AND SHALL BE DONE IN ACCORDANCE WITH SECTION 502.3.5.3 OF THE STANDARD SPECIFICATIONS.

SLAB FALSEWORK SHALL BE SUPPORTED ON PILES OR THE SUBSTRUCTURE UNLESS AN ALTERNATE METHOD IS APPROVED BY THE ENGINEER.

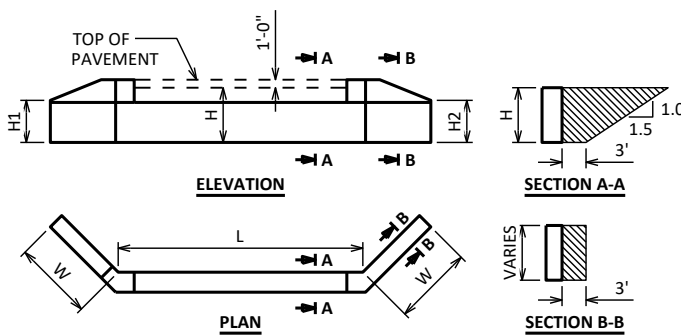
PROTECTIVE SURFACE TREATMENT TO BE APPLIED TO ENTIRE EXPOSED TOP OF SLAB, THE TOP AND EXTERIOR EXPOSED FACE OF WINGS AND FRONT FACE OF ABUTMENT TO 1'-0" PAST THE EDGE OF SLAB.

PIGMENTED SURFACE SEALER TO BE APPLIED TO THE FRONT FACE AND TOP OF PARAPET.



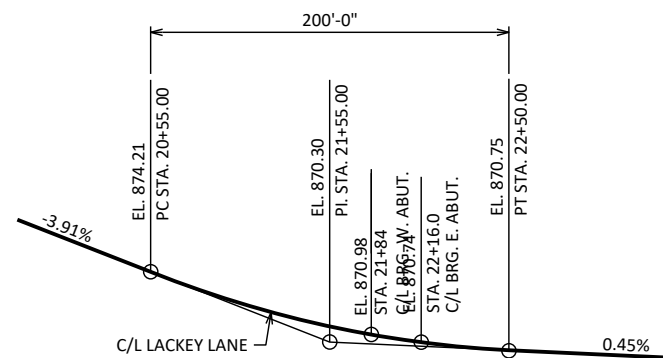
CROSS SECTION THRU ROADWAY

LOOKING UPSTATION
(PILING NOT SHOWN FOR CLARITY)

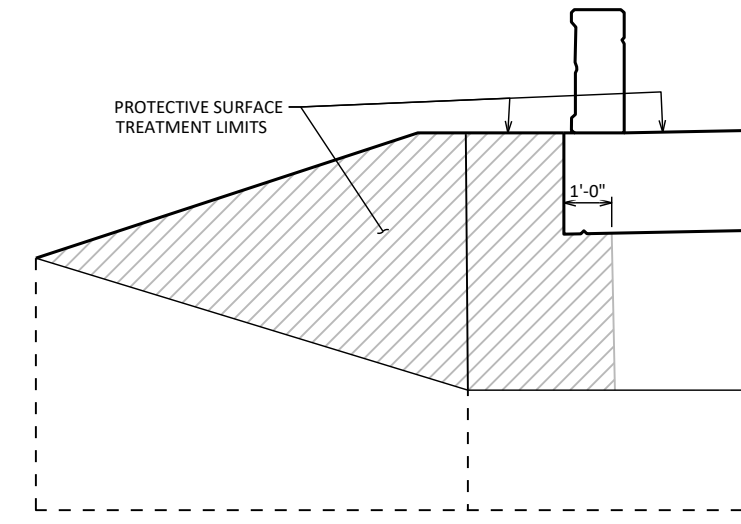


ABUTMENT BACKFILL DIAGRAM

- L = ABUTMENT BODY LENGTH AT BACKFACE (FT)
- H = AVERAGE ABUTMENT FILL HEIGHT (FT)
- H1 = WING 1 HEIGHT AT TIP (FT)
- H2 = WING 2 HEIGHT AT TIP (FT)
- W = WING LENGTH (FT)
- EF = EXPANSION FACTOR (1.20 FOR CY BID ITEMS AND 1.00 FOR TON BID ITEMS)
- $V_{CF} = (L)(3.0')(H) + (L)(0.5)(1.5H)(H) + (3')(0.5)(H1+H2+H+H)(W)$
- $V_{CY} = V_{CF}(EF)/27$
- $V_{TON} = V_{CY}(2.0)$



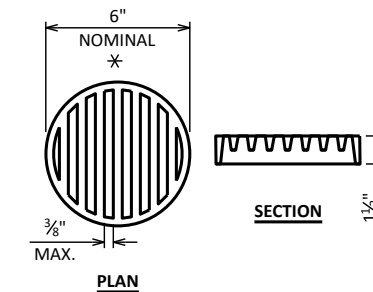
PROFILE GRADE LINE



PROTECTIVE SURFACE TREATMENT DETAILS

BENCH MARKS

| NO. | STATION | DESCRIPTION | ELEV. |
|-----|---------------------|------------------------------------------------------------------|--------|
| 5 | 22+10.53, 17.62' LT | X ON TOP OF NORTHEAST CONCRETE WINGWALL ON BRIDGE ON LACKEY LANE | 870.29 |

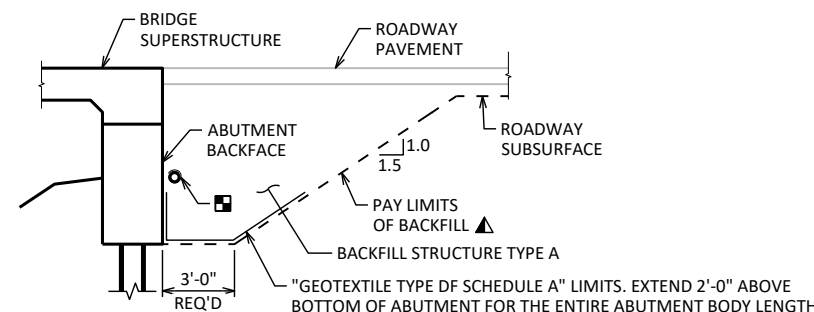


RODENT SHIELD DETAIL

* DIMENSIONS ARE APPROXIMATE. THE GRATE IS SIZED TO FIT INTO A PIPE COUPLING. ORIENT SO SLOTS ARE VERTICAL.

THE RODENT SHIELD, PIPE COUPLING AND SCREWS SHALL BE CONSIDERED INCIDENTAL TO THE BID ITEM "PIPE UNDERDRAIN WRAPPED 6-INCH".

THE RODENT SHIELD SHALL BE A PVC GRATE SIMILAR TO THIS DETAIL. THE GRATE IS COMMERCIALY AVAILABLE AS A FLOOR STRAINER. A PIPE COUPLING IS REQUIRED FOR THE ATTACHMENT OF THIS SHIELD TO THE EXPOSED END OF THE PIPE UNDERDRAIN. THE SHIELD SHALL BE FASTENED TO THE PIPE COUPLING WITH TWO OR MORE NO. 10 X 1-INCH STAINLESS STEEL SHEET METAL SCREWS.



TYPICAL SECTION THRU ABUTMENT

▲ BACKFILL PAY LIMITS. BACKFILL BEYOND PAY LIMITS SHALL BE INCIDENTAL TO EXCAVATION FOR STRUCTURES. LIMITS OF EXCAVATION SHALL BE DETERMINED BY THE CONTRACTOR.

■ PIPE UNDERDRAIN WRAPPED (6-INCH). SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. ATTACH RODENT SHIELD AT ENDS OF PIPE UNDERDRAIN.

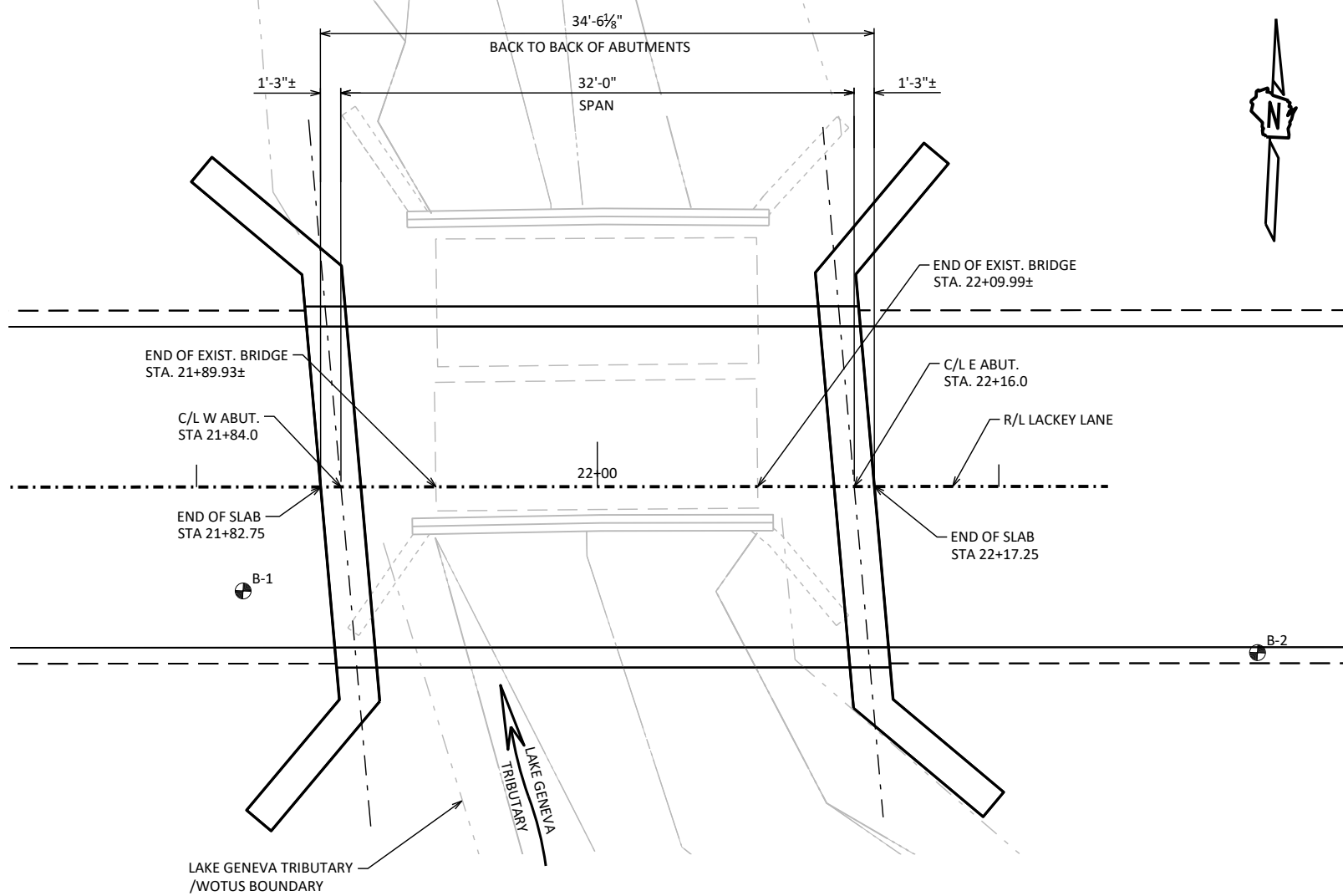
TOTAL ESTIMATED QUANTITIES

| BID ITEM NUMBER | BID ITEMS | UNIT | SUPER | WEST ABUT. | EAST ABUT. | TOTALS |
|-----------------|-----------------------------------------------------------|------|--------|------------|------------|------------|
| 203.0270.01 | REMOVING STRUCTURE OVER WATERWAY DEBRIS CAPTURE P-64-0906 | EACH | --- | --- | --- | 1 |
| 206.1001.01 | EXCAVATION FOR STRUCTURES BRIDGES B-64-0214 | EACH | --- | --- | --- | 1 |
| 210.1500 | BACKFILL STRUCTURE TYPE A | TON | --- | 115 | 115 | 230 |
| 502.0100 | CONCRETE MASONRY BRIDGES | CY | 53 | 22 | 22 | 97 |
| 502.3200 | PROTECTIVE SURFACE TREATMENT | SY | 77 | 14 | 14 | 105 |
| 502.3210 | PIGMENTED SURFACE SEALER | SY | 34 | --- | --- | 34 |
| 505.0400 | BAR STEEL REINFORCEMENT HS STRUCTURES | LB | --- | 1,740 | 1,740 | 3,480 |
| 505.0600 | BAR STEEL REINFORCEMENT HS COATED STRUCTURES | LB | 16,400 | 1,460 | 1,460 | 19,320 |
| 516.0500 | RUBBERIZED MEMBRANE WATERPROOFING | SY | --- | 5 | 5 | 10 |
| 517.1010.S | CONCRETE STAINING B-64-0214 | SF | 49 | --- | --- | 49 |
| 517.1015.S | CONCRETE STAINING MULTI-COLOR B-64-0214 | SF | 135 | --- | --- | 135 |
| 517.1050.S | ARCHITECTURAL SURFACE TREATMENT | SF | 135 | --- | --- | 135 |
| 526.0101.01 | TEMPORARY STRUCTURE (STA 42+00) | EACH | --- | --- | --- | 1 |
| 550.2104 | PILING CIP CONCRETE 10 3/4 X 0.25-INCH | LF | --- | 520 | 520 | 1,040 |
| 606.0300 | RIPRAP HEAVY | CY | --- | 36 | 36 | 72 |
| 612.0406 | PIPE UNDERDRAIN WRAPPED 6-INCH | LF | --- | 65 | 65 | 130 |
| 645.0111 | GEOTEXTILE TYPE DF SCHEDULE A | SY | --- | 39 | 39 | 78 |
| 645.0120 | GEOTEXTILE TYPE HR | SY | --- | 76 | 76 | 152 |
| NON-BID ITEMS | | | | | | |
| | FILLER | SIZE | --- | --- | --- | 1/2", 3/4" |

| NO. | DATE | REVISION | BY |
|----------------------------------------------------|------|------------|---------------|
| STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION | | | |
| STRUCTURE B-64-0214 | | | |
| DRAWN BY | | PLANS CK'D | |
| UKB | | BLB | |
| CROSS SECTION & QUANTITIES | | | SHEET 2 OF 10 |

| BORING # | DATE COMPLETED | NORTHING (Y) | EASTING (X) |
|----------|----------------|--------------|-------------|
| B-1 | 5-10-2023 | 324594.9756 | 777509.8840 |
| B-2 | 5-11-2023 | 324594.45412 | 777573.2206 |

BORINGS COMPLETED BY: GILES ENGINEERING ASSOCIATES, INC
REPORT COMPLETED BY: GILES ENGINEERING ASSOCIATES, INC
ALL COORDINATES REFERENCED TO LM-WCCS NAD 83(2011) WALWORTH COUNTY



NOTES:
BORING STATIONS AND OFFSETS ARE BASED ON C/L LACKEY LANE.
THE SUBBASE INFORMATION PRESENTED HEREIN IS AN ABBREVIATED VERSION OF THE INFORMATION PRESENTED IN THE GEOTECHNICAL ENGINEERING REPORT. REVIEW THE APPROPRIATE GEOTECHNICAL REPORT AND SOIL BORING LOGS FOR ADDITIONAL SUBSURFACE INFORMATION.

STATE PROJECT NUMBER
3846-00-75

MATERIAL SYMBOLS

| | | | | | |
|--|---------------------|--|-----------|--|-------------------|
| | ASPHALT | | TOPSOIL | | PEAT |
| | CONCRETE | | FILL | | GRAVEL |
| | SAND | | CLAY | | SILT |
| | BOULDERS OR COBBLES | | LIMESTONE | | BEDROCK (UNKNOWN) |
| | SHALE | | SANDSTONE | | IGNEOUS/META |

LEGEND OF BORING

BORING #/EL. STA./OFFSET

ST

0.25 (1) 17

F-C

COBBLE OR BOULDER

WEATHERED LIMESTONE

CORE RUN #1 - 24'-29'
REC=80%, RQD=72%

(1) UNCONFINED STRENGTH, AS DETERMINED BY A POCKET PENETROMETER (TSF)
(2) UNLESS OTHERWISE, SPECIFIED THE SPT 'N' VALUE IS BASED ON AASHTO T-206, STANDARD PENETRATION TEST. THE SPT 'N' VALUE PRESENTED HAS NOT BEEN CORRECTED FOR OVERBURDEN PRESSURE OR HAMMER EFFICIENCY.

GROUND WATER ELEVATION

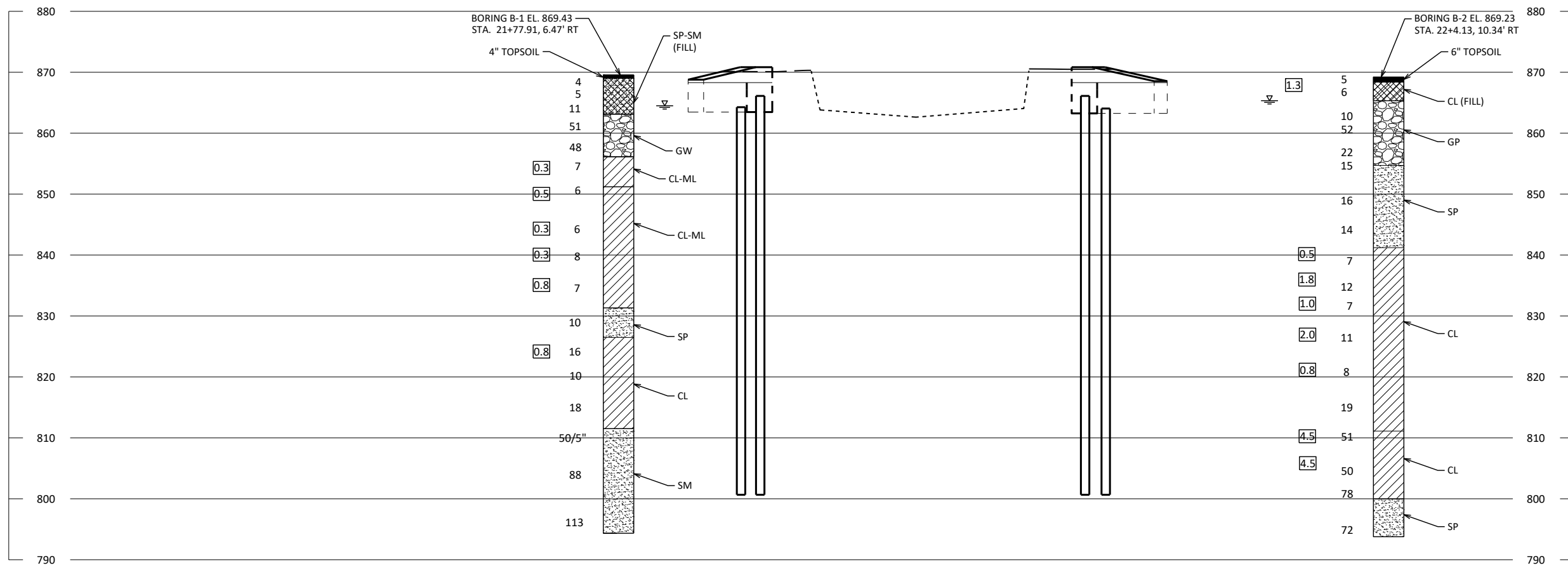
▽ AT TIME OF DRILLING
▼ END OF DRILLING
▽ AFTER DRILLING

ABBREVIATIONS

F-FINE M-MEDIUM C-COARSE ST-SHELBY TUBE

SUBSURFACE EXPLORATION FOR FOUNDATION DESIGN AND BIDDERS INFORMATION

BORINGS WERE COMPLETED AT POINTS APPROXIMATELY AS INDICATED ON THIS DRAWING TO OBTAIN INFORMATION CONCERNING THE CHARACTER OF SUBSURFACE MATERIALS FOUND AT THE SITE. BECAUSE THE INVESTIGATED DEPTHS ARE LIMITED AND THE AREA OF THE BORINGS IS VERY SMALL IN RELATION TO THE ENTIRE SITE, THE WISCONSIN DEPARTMENT OF TRANSPORTATION DOES NOT WARRANT SIMILAR SUBSURFACE CONDITIONS BELOW, BETWEEN, OR BEYOND THESE BORINGS. VARIATIONS IN SOIL CONDITIONS SHOULD BE EXPECTED AND FLUCTUATIONS IN GROUNDWATER LEVELS MAY OCCUR.

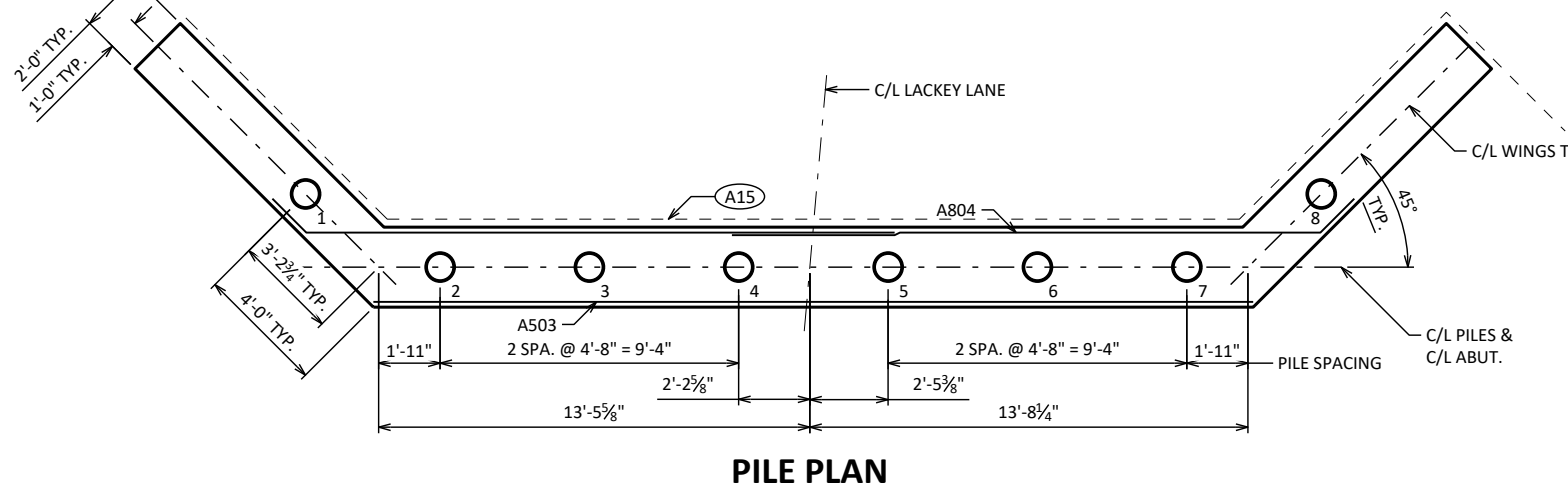
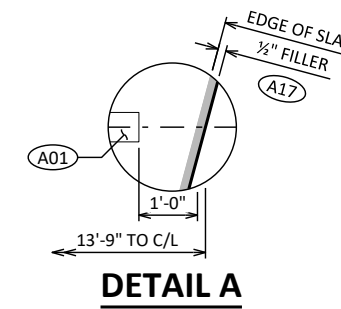
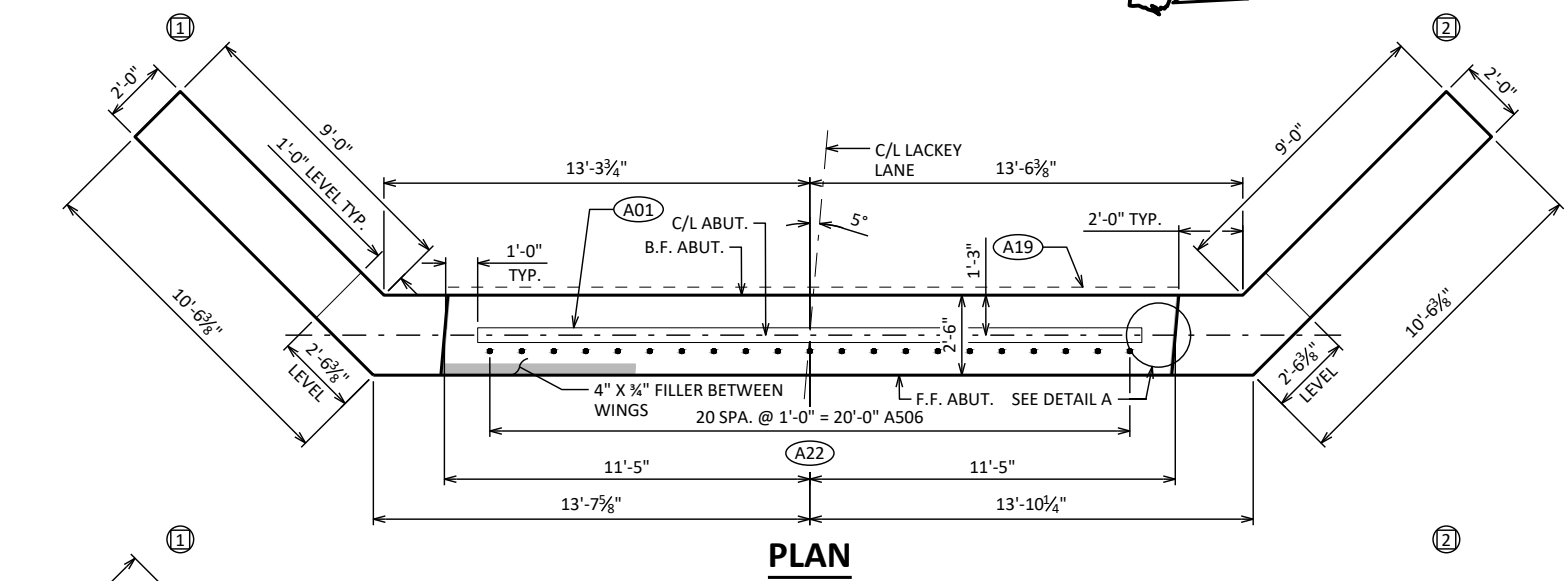
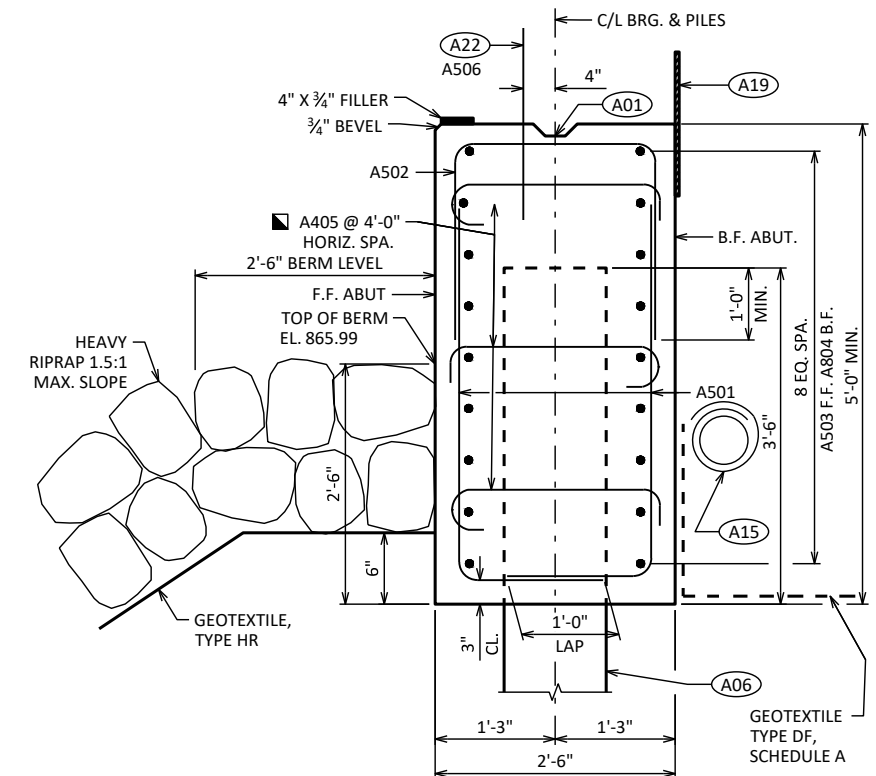
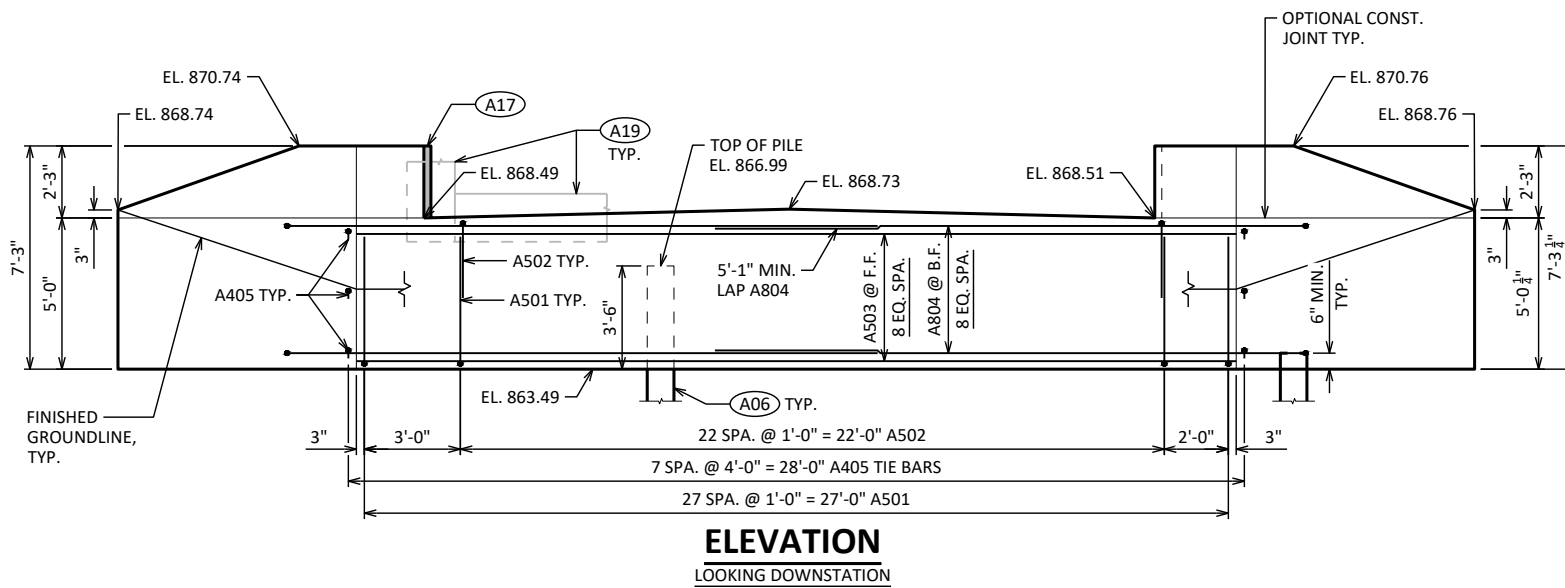


| NO. | DATE | REVISION | BY |
|----------------------------------------------------|------|----------------|---------------|
| STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION | | | |
| STRUCTURE B-64-0214 | | | |
| DRAWN BY UKB/SA | | PLANS CK'D BLB | |
| SUBSURFACE EXPLORATION | | | SHEET 3 OF 10 |

SCALE =

8

8



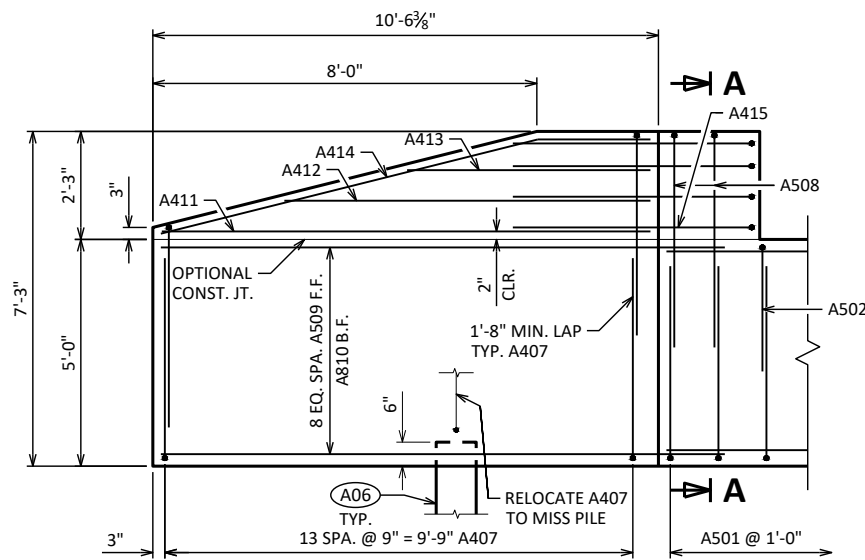
- (A01) CONST. JOINT: KEYWAY FORMED BY A BEVELED 2X6.
 - (A06) SUPPORT ABUTMENT ON CIP 10 3/4 x 0.25 PILING, ESTIMATED 65' LONG WITH A REQUIRED DRIVING RESISTANCE OF 90 TONS PER PILE.
 - (A15) PIPE UNDERDRAIN WRAPPED (6-INCH). SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. RODENT SHIELD REQUIRED.
 - (A17) 1/2" FILLER: SEAL ALL EXPOSED HORIZ. & VERT. SURFACES OF 1/2" FILLER WITH NON-STAINING GRAY NON-BITUMINOUS JOINT SEALER. (1" DEEP AND HOLD 3/8" BELOW SURFACE OF CONCRETE). EXTEND SEALER 3" BELOW GUTTER LINE AT INSIDE FACE.
 - (A19) 18" RUBBERIZED MEMBRANE WATERPROOFING. SEAL ALL HORIZ. & VERT. JOINTS AT BACKFACE.
 - (A22) A506 BARS SPACED @ 1'-0" CNTRS. MAY BE PLACED AFTER CONCRETE IS POURED BUT BEFORE INITIAL SET HAS TAKEN PLACE. (EMBED 1'-0" INTO CONC.)
- ALTERNATE THE POSITION OF THE 90° AND 180° HOOKS AT EACH VERTICAL LAYER OF TIES.

| NO. | DATE | REVISION | BY |
|----------------------------------------------------|------|----------------|----|
| STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION | | | |
| STRUCTURE B-64-0214 | | | |
| DRAWN BY SMA | | PLANS CK'D BLB | |
| WEST ABUTMENT | | SHEET 4 of 10 | |

BILL OF BARS

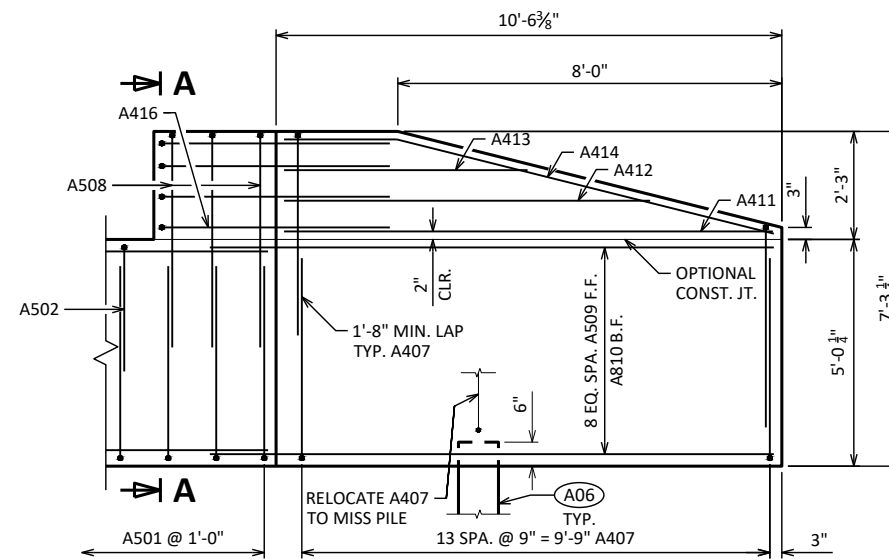
NOTE: THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE.

| BAR MARK | COAT | NO. REQ'D. | LENGTH | BENT | BAR SERIES | LOCATION |
|----------|------|------------|--------|------|------------|--------------------------------|
| A501 | | 56 | 5'-6" | X | | ABUT BODY STIRRUPS |
| A502 | | 23 | 7'-1" | X | | ABUT BODY STIRRUPS - TOP U-BAR |
| A503 | | 9 | 27'-6" | | | ABUT BODY HORIZ. - F.F. |
| A804 | | 18 | 19'-8" | X | | ABUT BODY HORIZ. - B.F. |
| A405 | | 24 | 3'-0" | X | | ABUT BODY TIE BARS |
| A506 | X | 21 | 2'-0" | | | ABUT BODY DOWEL BARS |
| A407 | X | 56 | 9'-10" | X | | WING STIRRUPS |
| A508 | X | 5 | 10'-9" | X | | WING CORNER STIRRUPS |
| A509 | X | 18 | 11'-9" | X | | WING LOWER HORIZ. - F.F. |
| A810 | X | 18 | 13'-3" | X | | WING LOWER HORIZ. - B.F. |
| A411 | X | 4 | 10'-1" | | | WING UPPER HORIZ. |
| A412 | X | 4 | 7'-6" | | | WING UPPER HORIZ. |
| A413 | X | 4 | 5'-0" | | | WING UPPER HORIZ. |
| A414 | X | 4 | 9'-8" | X | | WING TOP HORIZ. |
| A415 | X | 4 | 8'-1" | X | | WING 1 UPPER HORIZ. CORNER |
| A416 | X | 4 | 8'-7" | X | | WING 2 UPPER HORIZ. CORNER |



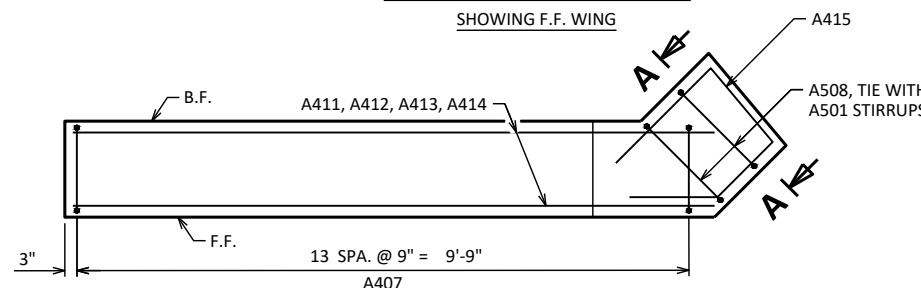
WING 1 ELEVATION

SHOWING F.F. WING



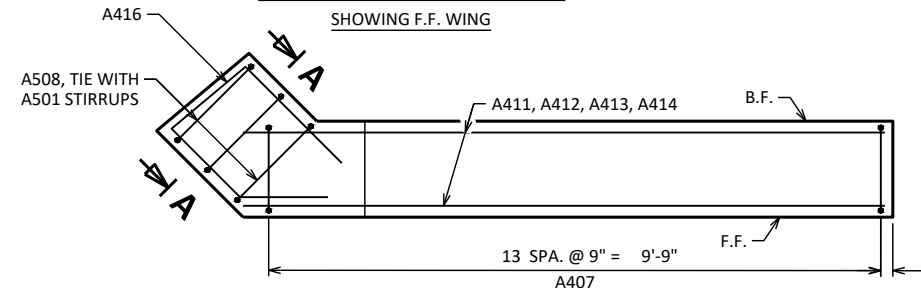
WING 2 ELEVATION

SHOWING F.F. WING



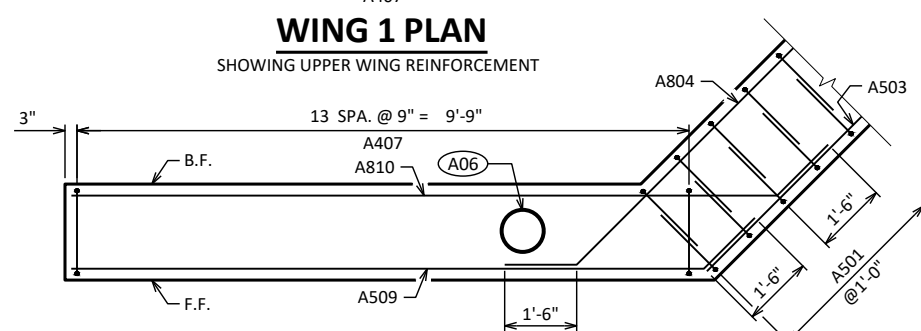
WING 1 PLAN

SHOWING UPPER WING REINFORCEMENT



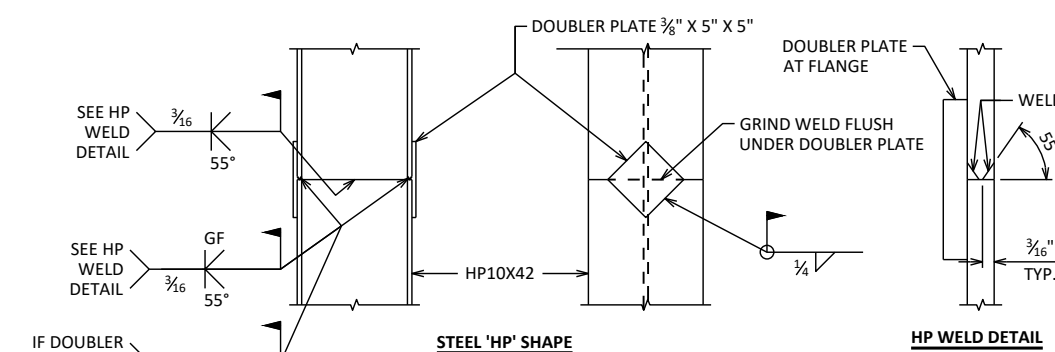
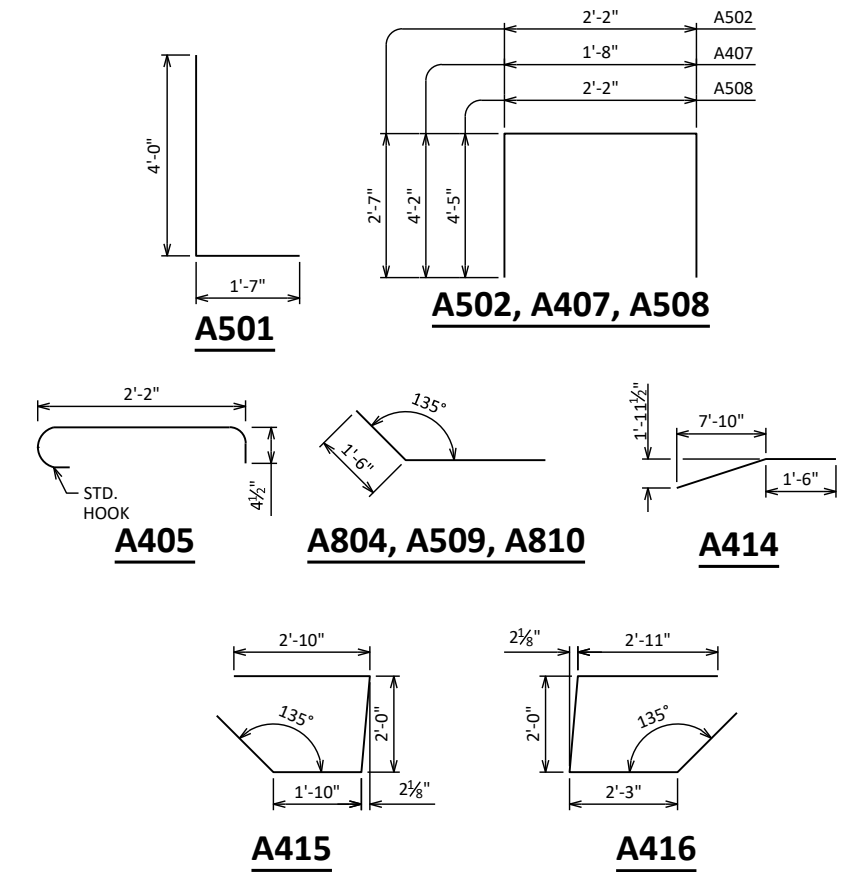
WING 2 PLAN

SHOWING UPPER WING REINFORCEMENT

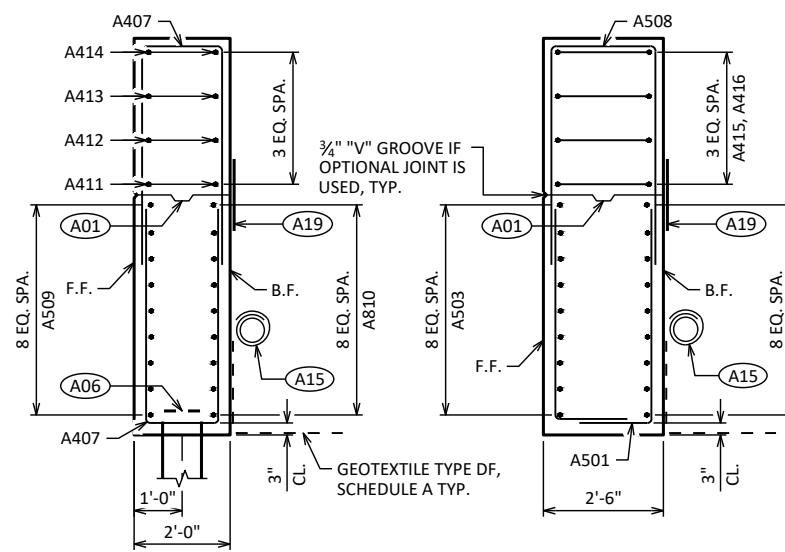


WING 1 PLAN

SHOWING LOWER WING REINFORCEMENT
WING 2 SIMILAR



'HP' PILE DETAILS



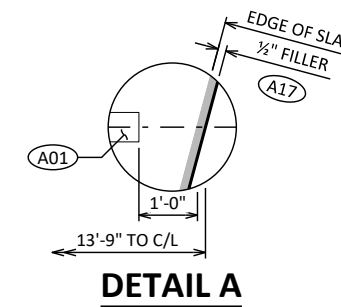
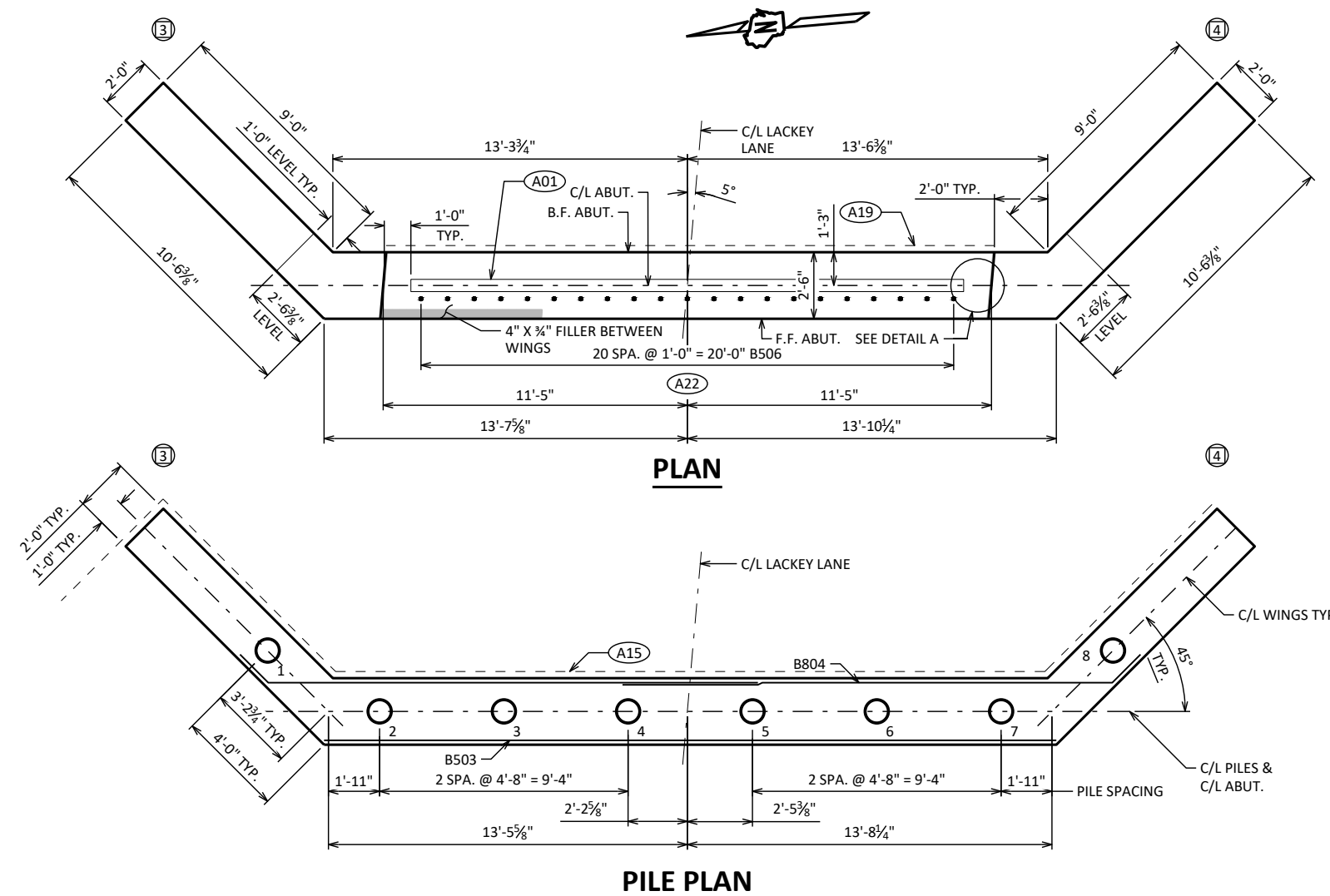
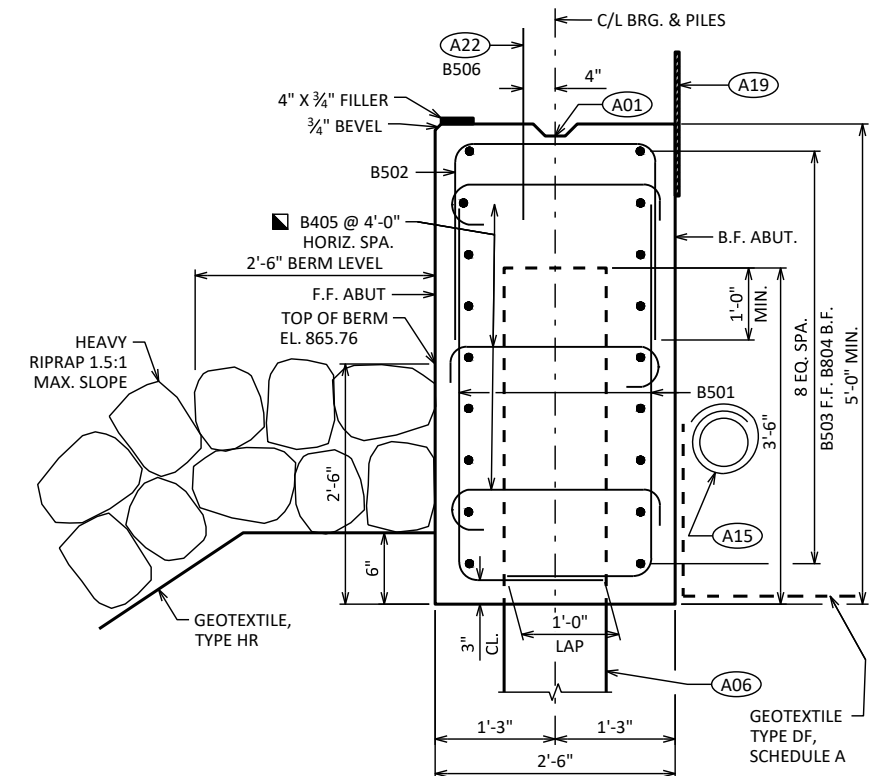
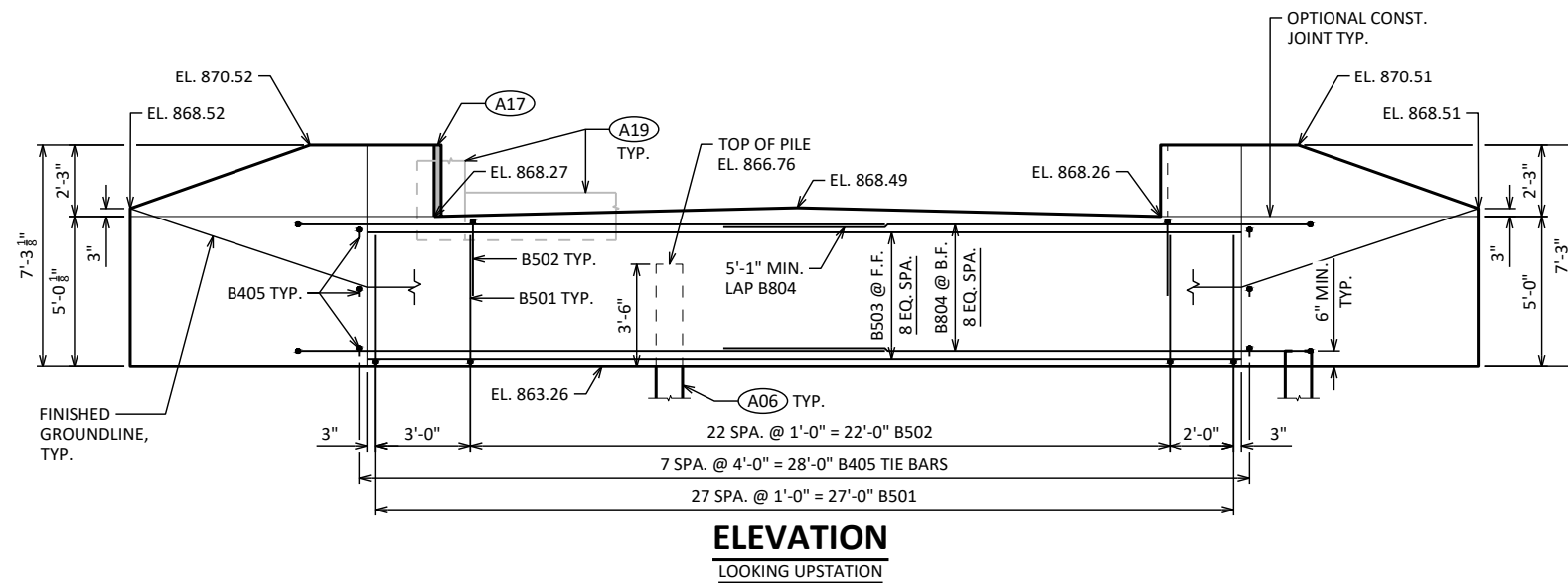
SECTION THRU WING 1

TYPICAL BOTH WINGS

SECTION A-A

- (A01) OPTIONAL CONST. JOINT: KEYWAY FORMED BY A BEVELED 2X6. PROVIDE 3/4" "V" GROOVE ON F.F. OF WINGWALL IF JOINT IS USED.
- (A06) SUPPORT ABUTMENT ON CIP 10 3/4 x 0.25 PILING, ESTIMATED 65' LONG WITH A REQUIRED DRIVING RESISTANCE OF 90 TONS PER PILE.
- (A15) PIPE UNDERDRAIN WRAPPED (6-INCH). SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. RODENT SHIELD REQUIRED.
- (A19) 18" RUBBERIZED MEMBRANE WATERPROOFING, ONLY IF OPTIONAL CONSTRUCTION JOINT IS USED. COST INCIDENTAL TO BID ITEM "CONCRETE MASONRY STRUCTURES".

| NO. | DATE | REVISION | BY |
|----------------------------------------------------|------|----------|---------------|
| STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION | | | |
| STRUCTURE B-64-0214 | | | |
| DRAWN BY | | SMA | PLANS CK'D |
| BY | | BLB | BLB |
| WEST ABUTMENT DETAILS | | | SHEET 5 OF 10 |



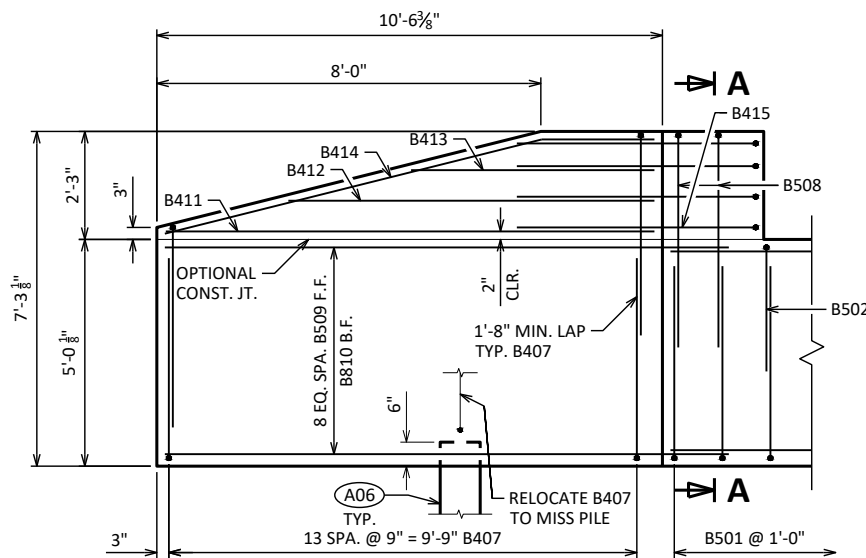
- (A01) CONST. JOINT: KEYWAY FORMED BY A BEVELED 2X6.
 - (A06) SUPPORT ABUTMENT ON CIP 10 3/4 x 0.25 PILING, ESTIMATED 65' LONG WITH A REQUIRED DRIVING RESISTANCE OF 90 TONS PER PILE.
 - (A15) PIPE UNDERDRAIN WRAPPED (6-INCH). SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. RODENT SHIELD REQUIRED.
 - (A17) 1/2" FILLER: SEAL ALL EXPOSED HORIZ. & VERT. SURFACES OF 1/2" FILLER WITH NON-STAINING GRAY NON-BITUMINOUS JOINT SEALER. (1" DEEP AND HOLD 3/8" BELOW SURFACE OF CONCRETE). EXTEND SEALER 3" BELOW GUTTER LINE AT INSIDE FACE.
 - (A19) 18" RUBBERIZED MEMBRANE WATERPROOFING. SEAL ALL HORIZ. & VERT. JOINTS AT BACKFACE.
 - (A22) B506 BARS SPACED @ 1'-0" CNTRS. MAY BE PLACED AFTER CONCRETE IS POURED BUT BEFORE INITIAL SET HAS TAKEN PLACE. (EMBED 1'-0" INTO CONC.)
- ALTERNATE THE POSITION OF THE 90° AND 180° HOOKS AT EACH VERTICAL LAYER OF TIES.

| NO. | DATE | REVISION | BY |
|----------------------------------------------------|------|---------------|----------------|
| STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION | | | |
| STRUCTURE B-64-0214 | | | |
| DRAWN BY | | SMA | PLANS CK'D BLB |
| EAST ABUTMENT | | SHEET 6 OF 10 | |

BILL OF BARS

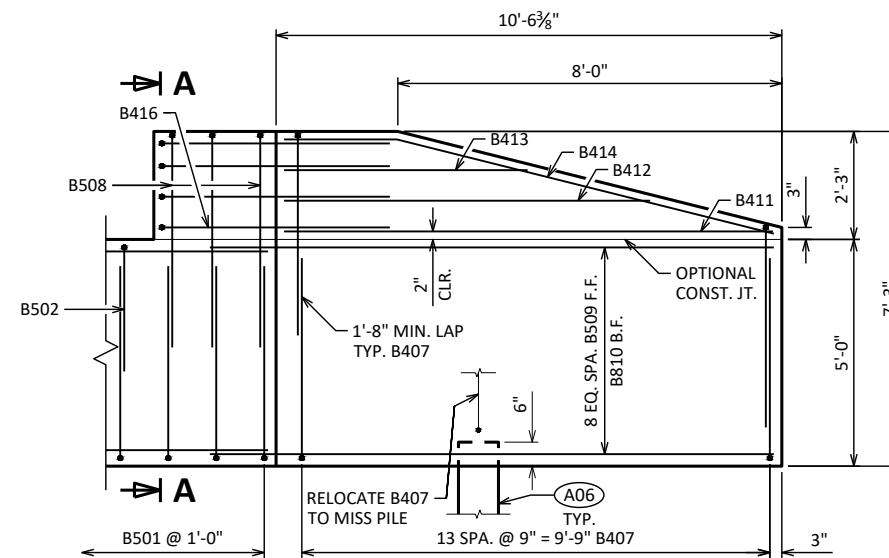
NOTE: THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE.

| BAR MARK | COAT | NO. REQ'D. | LENGTH | BENT | BAR SERIES | LOCATION |
|----------|------|------------|--------|------|------------|--------------------------------|
| B501 | | 56 | 5'-6" | X | | ABUT BODY STIRRUPS |
| B502 | | 23 | 7'-1" | X | | ABUT BODY STIRRUPS - TOP U-BAR |
| B503 | | 9 | 27'-6" | | | ABUT BODY HORIZ. - F.F. |
| B804 | | 18 | 19'-8" | X | | ABUT BODY HORIZ. - B.F. |
| B405 | | 24 | 3'-0" | X | | ABUT BODY TIE BARS |
| B506 | X | 21 | 2'-0" | | | ABUT BODY DOWEL BARS |
| B407 | X | 56 | 9'-10" | X | | WING STIRRUPS |
| B508 | X | 5 | 10'-9" | X | | WING CORNER STIRRUPS |
| B509 | X | 18 | 11'-9" | X | | WING LOWER HORIZ. - F.F. |
| B810 | X | 18 | 13'-3" | X | | WING LOWER HORIZ. - B.F. |
| B411 | X | 4 | 10'-1" | | | WING UPPER HORIZ. |
| B412 | X | 4 | 7'-6" | | | WING UPPER HORIZ. |
| B413 | X | 4 | 5'-0" | | | WING UPPER HORIZ. |
| B414 | X | 4 | 9'-8" | X | | WING TOP HORIZ. |
| B415 | X | 4 | 8'-1" | X | | WING 3 UPPER HORIZ. CORNER |
| B416 | X | 4 | 8'-7" | X | | WING 4 UPPER HORIZ. CORNER |



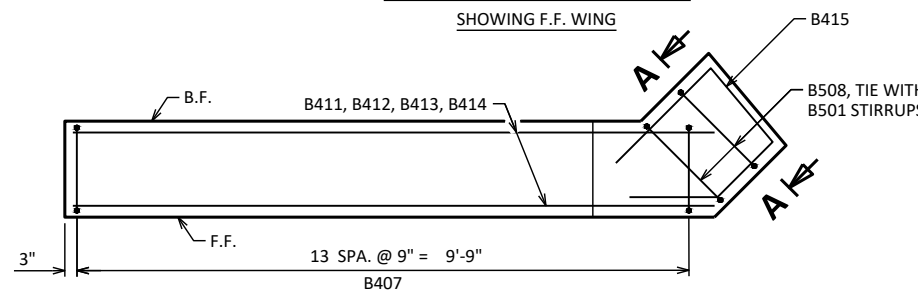
WING 3 ELEVATION

SHOWING F.F. WING



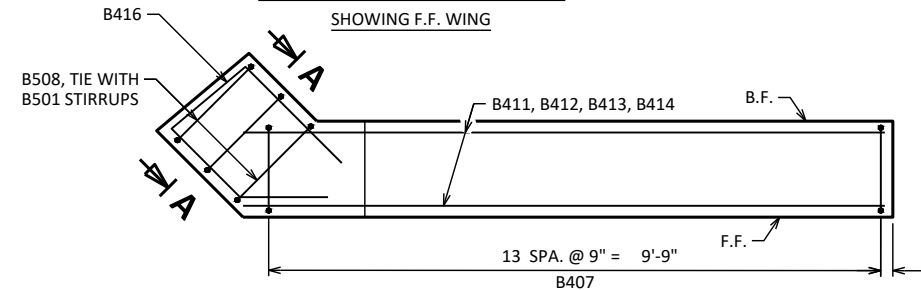
WING 4 ELEVATION

SHOWING F.F. WING



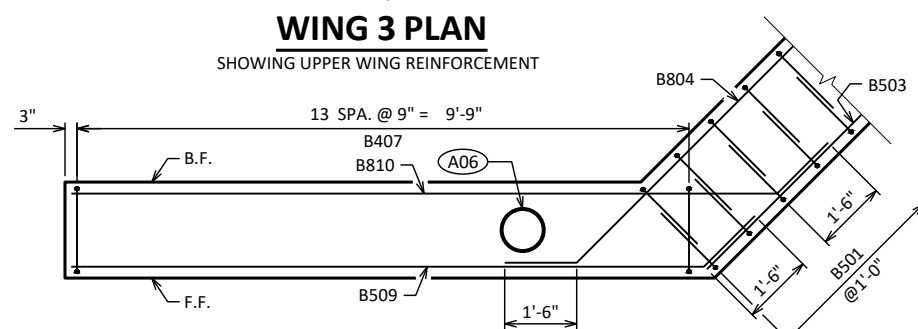
WING 3 PLAN

SHOWING UPPER WING REINFORCEMENT



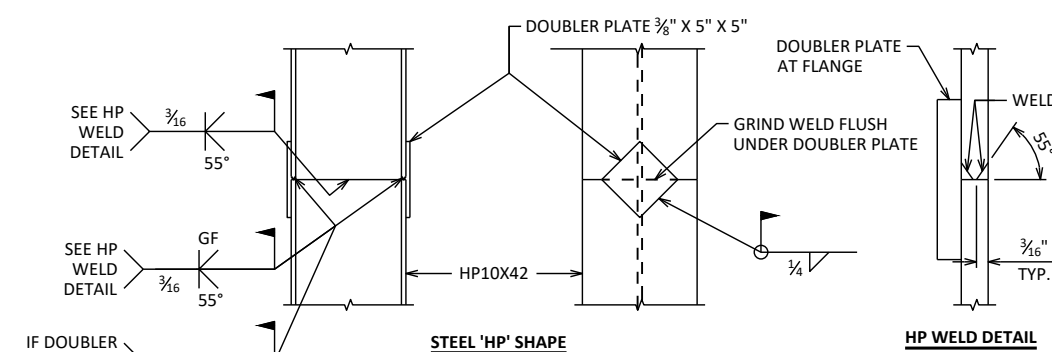
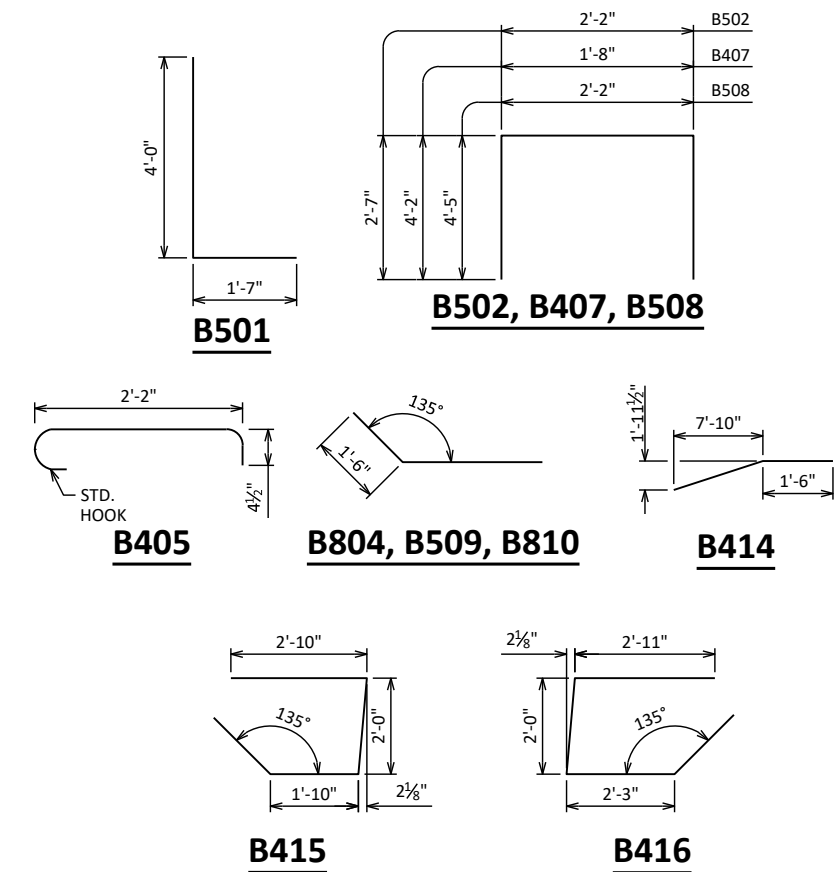
WING 4 PLAN

SHOWING UPPER WING REINFORCEMENT

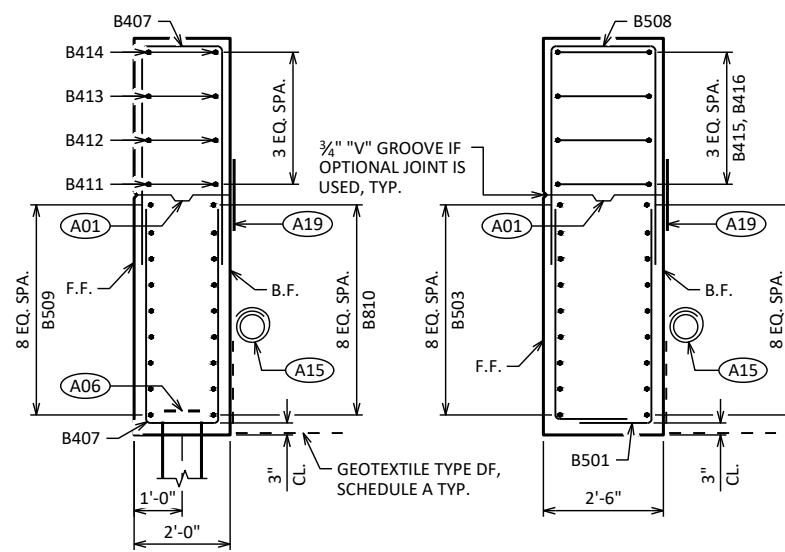


WING 3 PLAN

SHOWING LOWER WING REINFORCEMENT WING 4 SIMILAR



'HP' PILE DETAILS



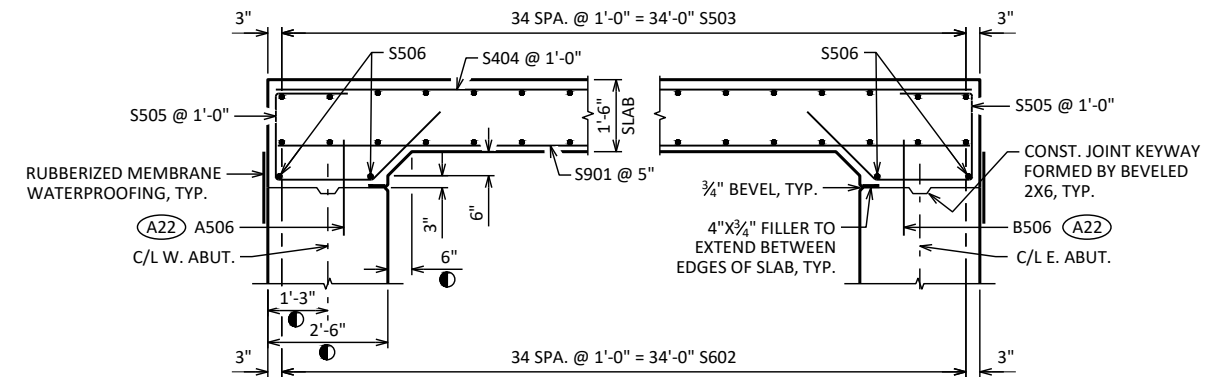
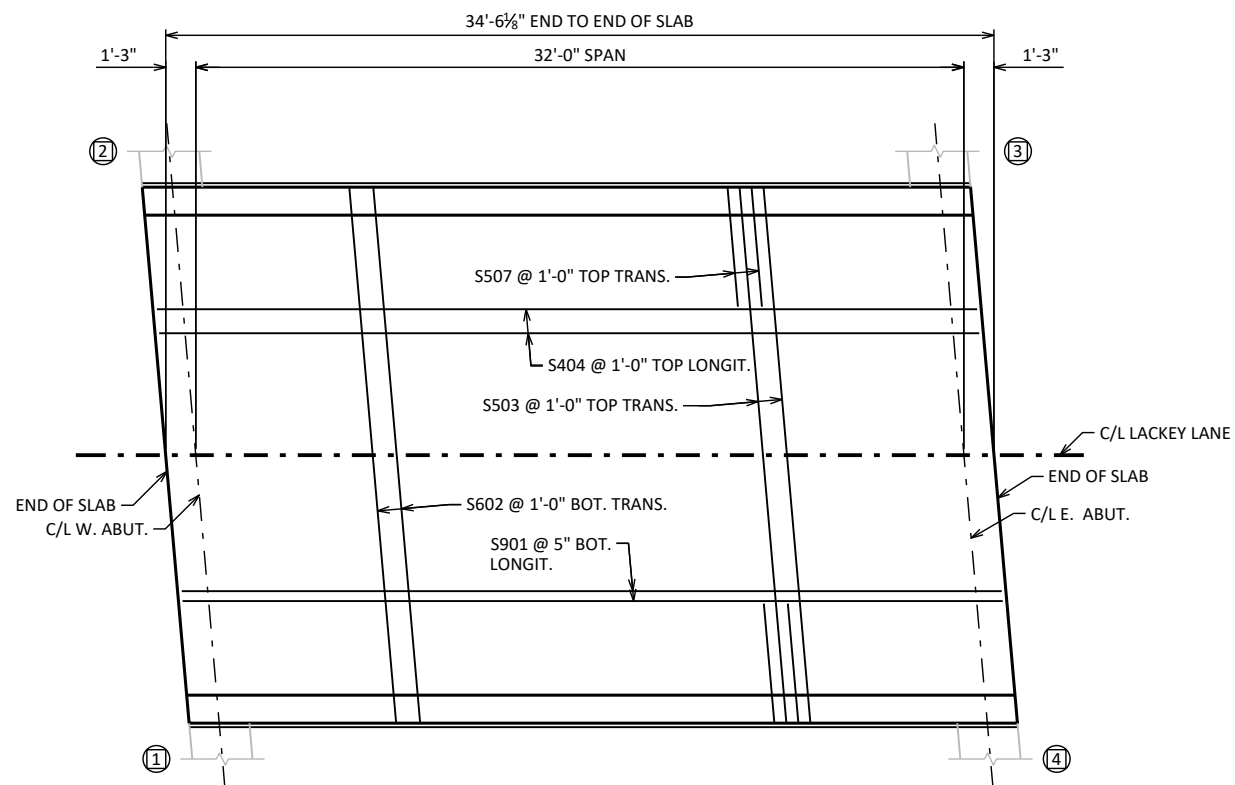
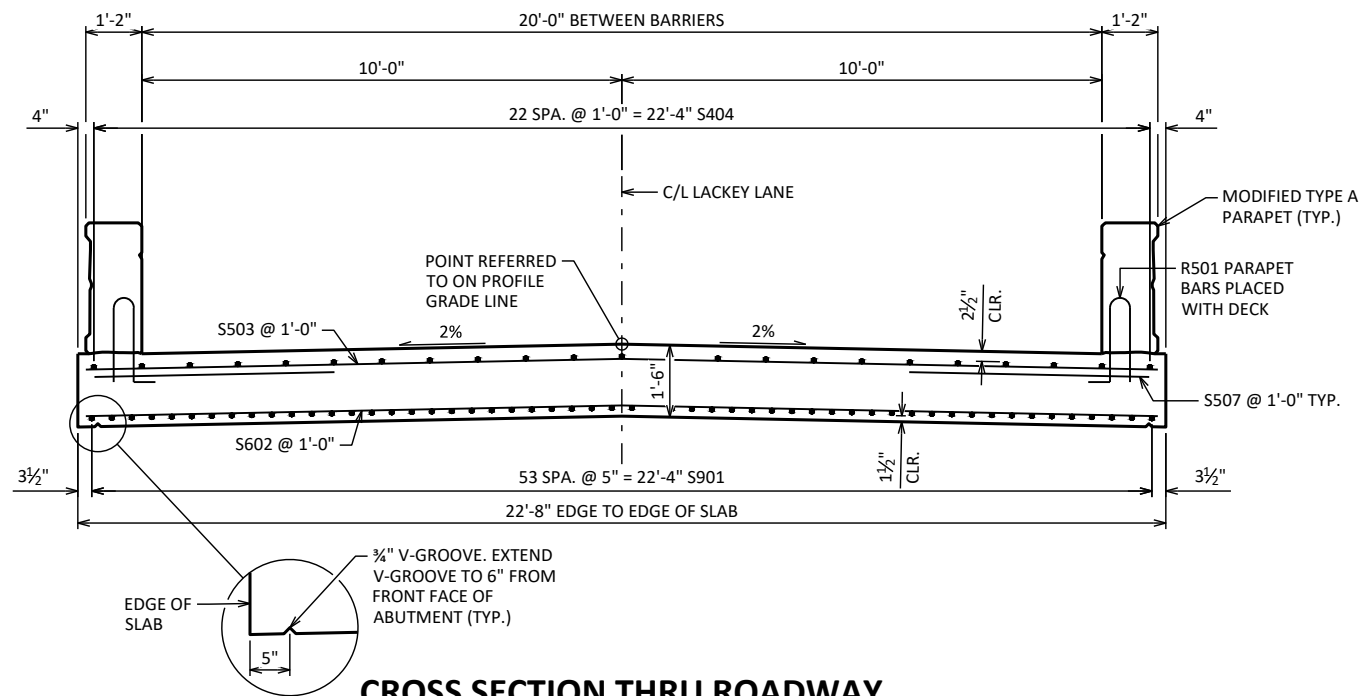
SECTION THRU WING 3

TYPICAL BOTH WINGS

SECTION A-A

- A01 OPTIONAL CONST. JOINT: KEYWAY FORMED BY A BEVELED 2X6. PROVIDE 3/4" "V" GROOVE ON F.F. OF WINGWALL IF JOINT IS USED.
- A06 SUPPORT ABUTMENT ON CIP 10 3/4 x 0.25 PILING, ESTIMATED 65' LONG WITH A REQUIRED DRIVING RESISTANCE OF 90 TONS PER PILE.
- A15 PIPE UNDERDRAIN WRAPPED (6-INCH). SLOPE 0.5% MIN. TO SUITABLE DRAINAGE. RODENT SHIELD REQUIRED.
- A19 18" RUBBERIZED MEMBRANE WATERPROOFING, ONLY IF OPTIONAL CONSTRUCTION JOINT IS USED. COST INCIDENTAL TO BID ITEM "CONCRETE MASONRY STRUCTURES".

| NO. | DATE | REVISION | BY |
|----------------------------------------------------|------|----------------|----|
| STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION | | | |
| STRUCTURE B-64-0214 | | | |
| DRAWN BY SMA | | PLANS CK'D BLB | |
| EAST ABUTMENT DETAILS | | SHEET 7 OF 10 | |

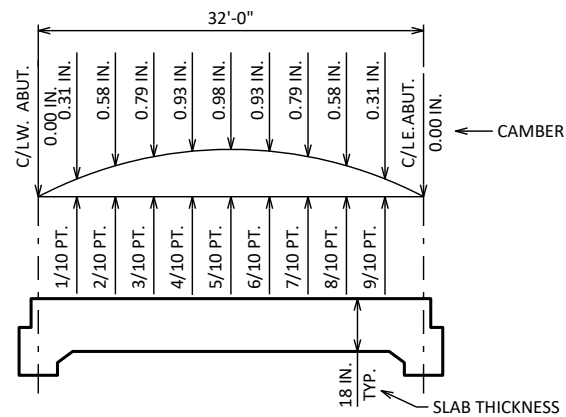


DIMENSIONS ARE GIVEN PARALLEL TO ϵ ROADWAY UNLESS OTHERWISE NOTED.

- MEASURED NORMAL TO THE ϵ OF ABUTMENT. DIMENSIONS ARE TYPICAL FOR BOTH ABUTMENTS.
- (A22) A506, B506 BARS SPACED @ 1'-0" CNTRS. MAY BE PLACED AFTER CONCRETE IS POURED BUT BEFORE INITIAL SET HAS TAKEN PLACE. (EMBED 1'-0" INTO CONC.)

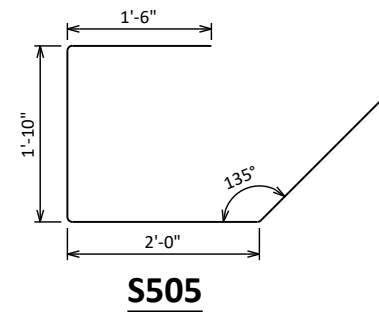


| NO. | DATE | REVISION | BY |
|----------------------------------------------------|------|----------------|---------------|
| STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION | | | |
| STRUCTURE B-64-0214 | | | |
| DRAWN BY SMA | | PLANS CK'D BLB | |
| SUPERSTRUCTURE | | | SHEET 8 OF 10 |



CAMBER AND SLAB THICKNESS DIAGRAM

CAMBER SHOWN IS BASED ON 3 TIMES DEAD LOAD DEFLECTIONS. CAMBER SPANS AS SHOWN TO PROVIDE FOR DEAD LOAD DEFLECTION AND FUTURE CREEP. CAMBER DOES NOT INCLUDE ALLOWANCE FOR FORM SETTLEMENT. PARAPETS PLACED ON TOP OF THE SLAB SHALL BE POURED AFTER FALSEWORK HAS BEEN RELEASED, EXCEPT FOR STAGED CONSTRUCTION.



BILL OF BARS

NOTE: THE FIRST OR FIRST TWO DIGITS OF THE BAR MARK SIGNIFIES THE BAR SIZE.

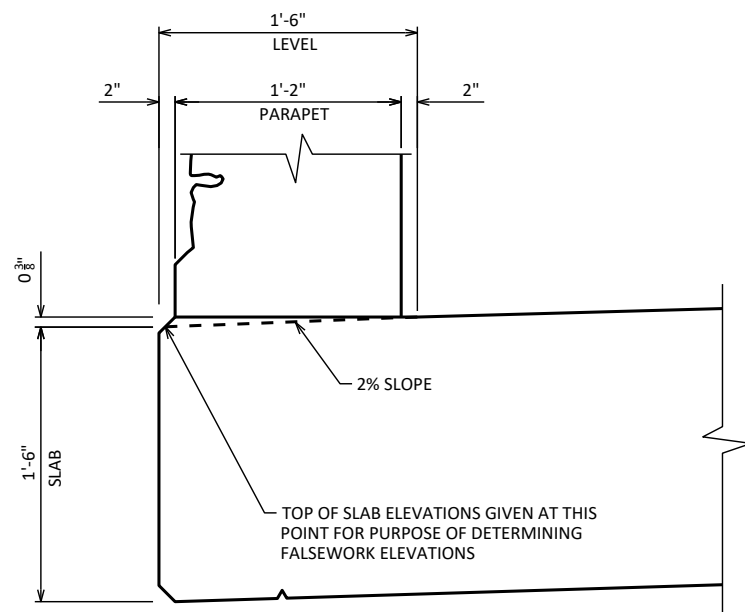
| BAR MARK | COAT | NO. REQ'D. | LENGTH | BENT | BAR SERIES | LOCATION |
|----------|------|------------|--------|------|------------|---------------------------------|
| S901 | X | 54 | 34'-2" | | | SLAB BOTTOM LONGITUDINAL |
| S602 | X | 35 | 22'-5" | | | SLAB BOTTOM TRANSVERSE |
| S503 | X | 35 | 22'-5" | | | SLAB TOP TRANSVERSE |
| S404 | X | 23 | 34'-2" | | | SLAB TOP LONGITUDINAL |
| S505 | X | 46 | 7'-1" | X | | ABUTMENT DIAPHRAGM STIRRUPS |
| S506 | X | 4 | 22'-5" | | | ABUTMENT DIAPHRAGM LONGITUDINAL |
| S507 | X | 68 | 5'-0" | | | SLAB TOP EDGE TRANSVERSE |
| | | | | | | |
| | | | | | | |
| | | | | | | |

TO DETERMINE FALSEWORK ELEVATION AT EDGE OF SLAB, CROWN OR REFERENCE LINE FOLLOW THIS PROCEDURE:

- LESS TOP OF SLAB ELEVATION AT FINAL GRADE (SEE SECTION A ON SHEET 10 OF 10 AND BELOW)
- LESS SLAB THICKNESS
- PLUS CAMBER
- PLUS FORM SETTLEMENT/DEFLECTION DUE TO PLACEMENT OF SLAB CONCRETE (TO BE COMPUTED BY THE CONTRACTOR)
- EQUALS TOP OF SLAB FALSEWORK ELEVATION

TOP OF SLAB ELEVATIONS

| LOCATION | C/L BRG. W. ABUT. | 1/10 PT. | 2/10 PT. | 3/10 PT. | 4/10 PT. | 5/10 PT. | 6/10 PT. | 7/10 PT. | 8/10 PT. | 9/10 PT. | C/L BRG. E. ABUT. |
|-----------------|-------------------|----------|----------|----------|----------|----------|----------|----------|----------|----------|-------------------|
| N. EDGE OF DECK | 870.76 | 870.73 | 870.70 | 870.67 | 870.64 | 870.61 | 870.59 | 870.57 | 870.55 | 870.53 | 870.52 |
| CROWN OR R/L | 870.98 | 870.95 | 870.91 | 870.89 | 870.86 | 870.83 | 870.81 | 870.79 | 870.77 | 870.75 | 870.74 |
| S. EDGE OF DECK | 870.74 | 870.71 | 870.68 | 870.65 | 870.62 | 870.60 | 870.58 | 870.56 | 870.54 | 870.52 | 870.51 |



SECTION A

SURVEY TOP OF SLAB ELEVATIONS

| LOCATION | ABUTMENT | 5/10 PT. | ABUTMENT |
|--------------|----------|----------|----------|
| N. GUTTER | | | |
| CROWN OR R/L | | | |
| S. GUTTER | | | |

PRIOR TO RELEASING SLAB FALSEWORK, TAKE TOP OF SLAB ELEVATIONS AT THE C/L OF ABUTMENTS, THE C/L OF PIERS AND AT 5/10 PTS. TO VERIFY CAMBER. TAKE ELEVATIONS ALONG GUTTER LINES AND CROWN OR R/L. RECORD THE ELEVATIONS IN THE ABOVE TABLE FOR THE "AS BUILT" PLANS.

NOTES

FILL IN THE TABLE OF "SURVEY TOP OF SLAB ELEVATIONS" FOR EACH SPAN ON AS BUILT PLANS.

TOP TRANSVERSE BARS IN SLAB SHALL BE SUPPORTED BY INDIVIDUAL BAR CHAIRS AT APPROXIMATELY 3'-0" CENTERS EACH WAY. BOTTOM LONGITUDINAL BARS SHALL BE SUPPORTED BY CONTINUOUS BAR CHAIRS AT APPROXIMATELY 4'-0" CENTERS.

ALL SLAB THICKNESS DIMENSIONS ARE MINIMUM. ANY TOLERANCES NECESSARY TO CORRECT CONSTRUCTION DISCREPANCIES ARE TO BE PLUS (+).

| NO. | DATE | REVISION | BY |
|-----|------|----------|----|
| | | | |

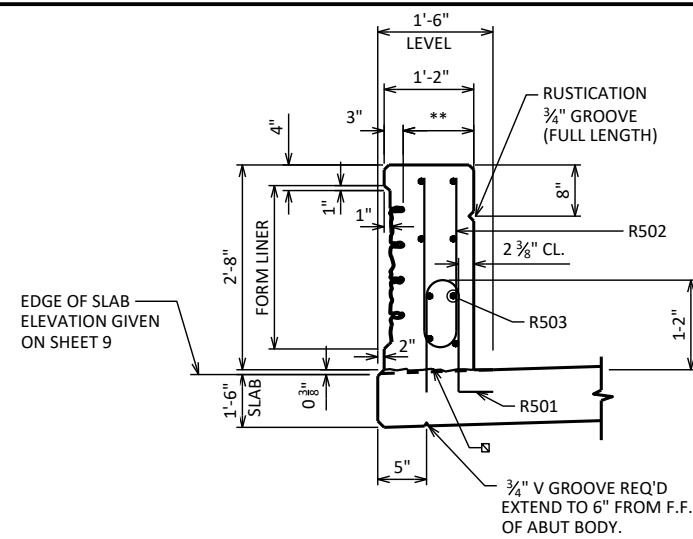
STATE OF WISCONSIN
DEPARTMENT OF TRANSPORTATION

STRUCTURE B-64-0214

DRAWN BY SMA PLANS CK'D BLB

SUPERSTRUCTURE DETAILS

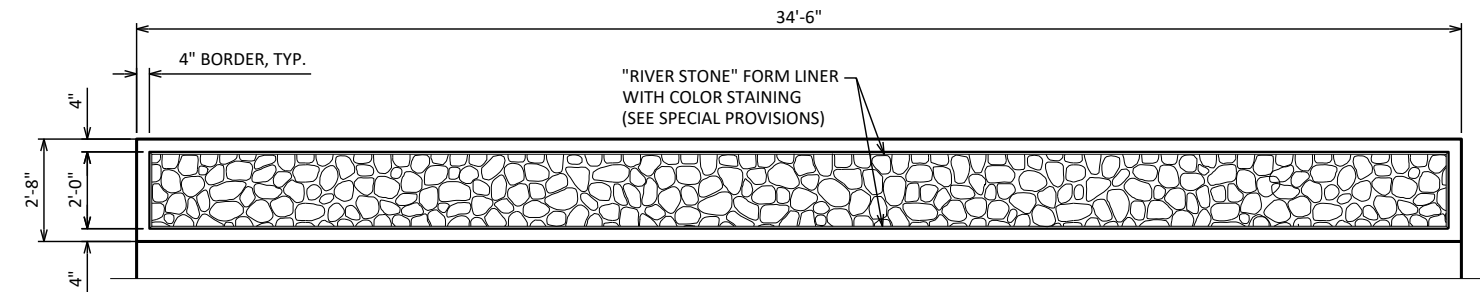
SHEET 9 OF 10



SECTION A

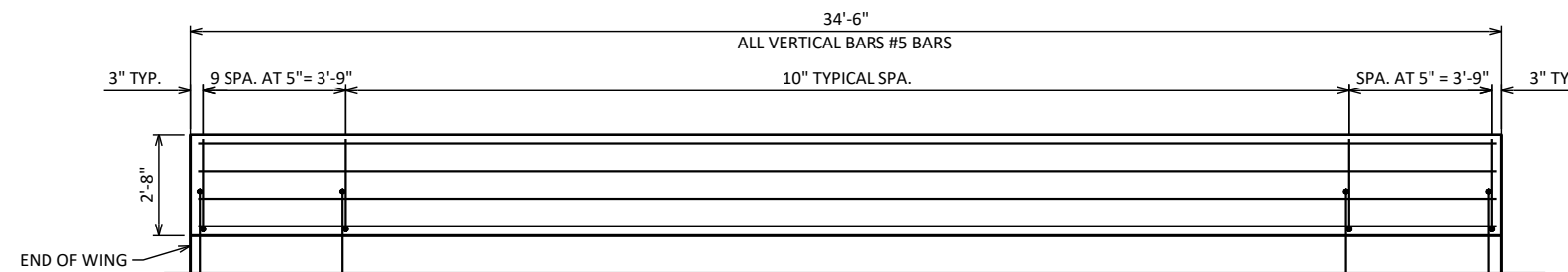
** 11" STRUCTURAL THICKNESS

EDGE OF SLAB
ELEVATION GIVEN
ON SHEET 9

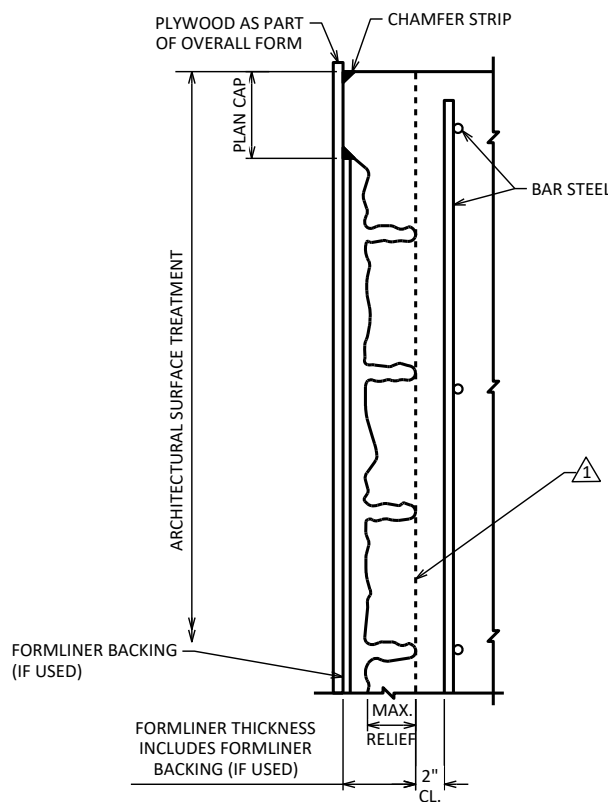


**VIEW SHOWING OUTSIDE FACE OF PARAPET
AND FORM LINER PATTERN**

TYPICAL OUTSIDE FACE OF BOTH HEADWALLS



**VIEW SHOWING OUTSIDE FACE OF PARAPET
& REINFORCEMENT**



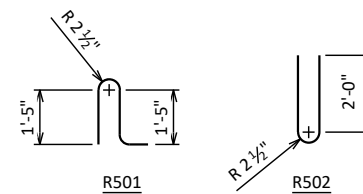
SECTION THRU FORMLINER

1 STRUCTURAL CONCRETE CAN ONLY BE ASSUMED TO THIS LINE. PROVIDE ADDITIONAL STRUCTURE SIZE AS NECESSARY TO MAINTAIN MINIMUM FULL STRUCTURAL CONCRETE DIMENSIONS AS INDICATED ON THE STANDARDS.

LEGEND

1 HORZ. CONST. JOINT-STRIKE OFF AS SHOWN AND LEAVE ROUGH.

| BILL OF BARS | | | | | | |
|--------------|------|-----------|--------|------|------------|---------------|
| BAR MARK | COAT | NO. REQ'D | LENGTH | BENT | BAR SERIES | LOCATION |
| R501 | X | 102 | 4'-4" | X | | PARAPET VERT. |
| R502 | X | 102 | 4'-9" | X | | PARAPET VERT. |
| R503 | X | 16 | 34'-2" | | | PARAPET HORZ. |



NOTES:

FINAL COLORATION APPLIED TO THE FORM LINER SURFACES SHALL SIMULATE THE APPEARANCE OF REAL STONE, INCLUDING NATURAL VARIATIONS IN COLOR. THE BORDER SURROUNDING THE FORM LINER AREA SHALL ALSO BE STAINED USING ONE OF THE COLORS FROM THE SELECTED PALETTE. THE TOWN WILL PROVIDE A COLOR RENDERING FOR REFERENCE UPON REQUEST.

| NO. | DATE | REVISION | BY |
|----------------------------------------------------|------|------------|----------------|
| STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION | | | |
| STRUCTURE B-64-0214 | | | |
| DRAWN BY | | PLANS CK'D | |
| UKB | | BLB | |
| MODIFIED TYPE A PARAPET DETAILS | | | SHEET 10 OF 10 |

Division 1 - LACKEY LANE - WEST OF STRUCTURE

| STATION | REAL STATION | DISTANCE | AREA (SF) | | | | | | INCREMENTAL VOL (CY) (UNADJUSTED) | | | CUMULATIVE VOL (CY) | | |
|----------|--------------|----------|-----------|-------------------------------------|-------|-----------|----------|------|-----------------------------------|-------------------------------------|------|---------------------|--------|---------------|
| | | | CUT | SALVAGED/UNUSABLE PAVEMENT MATERIAL | FILL | MARSH EXC | ROCK EXC | EBS | CUT | SALVAGED/UNUSABLE PAVEMENT MATERIAL | FILL | EXPANDED | | MASS ORDINATE |
| | | | | | | | | | | | | NOTE 1 | NOTE 2 | |
| 20+32.00 | 2032 | 0.00 | 0.00 | 5.67 | 0.00 | 0.00 | 0.00 | 0.00 | 0 | 0 | 0 | 0 | 0 | 0 |
| 20+35.00 | 2035 | 3.00 | 24.62 | 5.67 | 0.07 | 0.00 | 0.00 | 0.00 | 1 | 1 | 0 | 1 | 0 | 0 |
| 20+40.00 | 2040 | 5.00 | 22.96 | 5.67 | 0.32 | 0.00 | 0.00 | 0.00 | 4 | 1 | 0 | 5 | 0 | 3 |
| 20+45.00 | 2045 | 5.00 | 22.49 | 5.67 | 0.22 | 0.00 | 0.00 | 0.00 | 4 | 1 | 0 | 9 | 0 | 6 |
| 20+50.00 | 2050 | 5.00 | 22.59 | 5.67 | 0.32 | 0.00 | 0.00 | 0.00 | 4 | 1 | 0 | 13 | 0 | 9 |
| 20+55.00 | 2055 | 5.00 | 22.64 | 5.67 | 0.43 | 0.00 | 0.00 | 0.00 | 4 | 1 | 0 | 17 | 0 | 12 |
| 20+60.00 | 2060 | 5.00 | 22.18 | 5.67 | 0.51 | 0.00 | 0.00 | 0.00 | 4 | 1 | 0 | 21 | 0 | 15 |
| 20+65.00 | 2065 | 5.00 | 21.32 | 5.67 | 0.58 | 0.00 | 0.00 | 0.00 | 4 | 1 | 0 | 25 | 0 | 18 |
| 20+70.00 | 2070 | 5.00 | 20.35 | 5.67 | 0.72 | 0.00 | 0.00 | 0.00 | 4 | 1 | 0 | 29 | 0 | 21 |
| 20+75.00 | 2075 | 5.00 | 19.29 | 5.67 | 0.90 | 0.00 | 0.00 | 0.00 | 4 | 1 | 0 | 33 | 0 | 24 |
| 20+80.00 | 2080 | 5.00 | 18.12 | 5.67 | 1.09 | 0.00 | 0.00 | 0.00 | 3 | 1 | 0 | 36 | 0 | 26 |
| 20+85.00 | 2085 | 5.00 | 16.83 | 5.67 | 1.26 | 0.00 | 0.00 | 0.00 | 3 | 1 | 0 | 39 | 0 | 28 |
| 20+90.00 | 2090 | 5.00 | 15.45 | 5.67 | 1.46 | 0.00 | 0.00 | 0.00 | 3 | 1 | 0 | 42 | 0 | 30 |
| 20+95.00 | 2095 | 5.00 | 14.76 | 5.67 | 1.67 | 0.00 | 0.00 | 0.00 | 3 | 1 | 0 | 45 | 0 | 32 |
| 21+00.00 | 2100 | 5.00 | 14.05 | 5.67 | 2.10 | 0.00 | 0.00 | 0.00 | 3 | 1 | 0 | 48 | 0 | 34 |
| 21+05.00 | 2105 | 5.00 | 13.13 | 5.67 | 2.68 | 0.00 | 0.00 | 0.00 | 3 | 1 | 0 | 51 | 0 | 36 |
| 21+10.00 | 2110 | 5.00 | 11.91 | 5.67 | 3.41 | 0.00 | 0.00 | 0.00 | 2 | 1 | 1 | 53 | 1 | 36 |
| 21+15.00 | 2115 | 5.00 | 10.59 | 5.67 | 4.33 | 0.00 | 0.00 | 0.00 | 2 | 1 | 1 | 55 | 2 | 36 |
| 21+20.00 | 2120 | 5.00 | 9.34 | 5.67 | 5.53 | 0.00 | 0.00 | 0.00 | 2 | 1 | 1 | 57 | 3 | 36 |
| 21+25.00 | 2125 | 5.00 | 8.26 | 5.67 | 7.10 | 0.00 | 0.00 | 0.00 | 2 | 1 | 1 | 59 | 4 | 36 |
| 21+30.00 | 2130 | 5.00 | 7.13 | 5.67 | 8.85 | 0.00 | 0.00 | 0.00 | 1 | 1 | 1 | 60 | 6 | 35 |
| 21+35.00 | 2135 | 5.00 | 5.96 | 5.67 | 10.87 | 0.00 | 0.00 | 0.00 | 1 | 1 | 2 | 61 | 8 | 32 |
| 21+40.00 | 2140 | 5.00 | 4.94 | 5.67 | 13.37 | 0.00 | 0.00 | 0.00 | 1 | 1 | 2 | 62 | 10 | 30 |
| 21+45.00 | 2145 | 5.00 | 5.56 | 5.67 | 13.81 | 0.00 | 0.00 | 0.00 | 1 | 1 | 3 | 63 | 13 | 27 |
| 21+50.00 | 2150 | 5.00 | 6.15 | 5.67 | 13.41 | 0.00 | 0.00 | 0.00 | 1 | 1 | 3 | 64 | 17 | 24 |
| 21+55.00 | 2155 | 5.00 | 6.71 | 5.67 | 13.15 | 0.00 | 0.00 | 0.00 | 1 | 1 | 2 | 65 | 19 | 21 |
| 21+60.00 | 2160 | 5.00 | 7.24 | 5.67 | 13.01 | 0.00 | 0.00 | 0.00 | 1 | 1 | 2 | 66 | 21 | 19 |
| 21+65.00 | 2165 | 5.00 | 7.72 | 5.67 | 12.88 | 0.00 | 0.00 | 0.00 | 1 | 1 | 2 | 67 | 23 | 17 |
| 21+70.00 | 2170 | 5.00 | 8.13 | 5.67 | 12.71 | 0.00 | 0.00 | 0.00 | 1 | 1 | 2 | 68 | 25 | 15 |
| 21+75.00 | 2175 | 5.00 | 8.46 | 5.67 | 12.50 | 0.00 | 0.00 | 0.00 | 2 | 1 | 2 | 70 | 28 | 14 |
| 21+80.00 | 2180 | 5.00 | 8.71 | 5.67 | 12.28 | 0.00 | 0.00 | 0.00 | 2 | 1 | 2 | 72 | 30 | 12 |
| 21+82.00 | 2182 | 2.00 | 9.53 | 5.67 | 12.07 | 0.00 | 0.00 | 0.00 | 1 | 0 | 1 | 73 | 31 | 12 |

| NOTES: | |
|-----------------------------------------|--------------------------------------------------------------------------------------------------------------------------------|
| 1 - CUT | CUT INCLUDES SALVAGED/UNUSABLE PAVEMENT MATERIAL |
| 2 - SALVAGED/UNUSABLE PAVEMENT MATERIAL | THIS DOES NOT SHOW UP IN CROSS SECTIONS |
| 3 - FILL | DOES NOT INCLUDE UNUSABLE PAVEMENT EXC VOLUME |
| 8 - MASS ORDINATE | PLUS QUANTITY INDICATES AN EXCESS OF MATERIAL WITHIN THE DIVISION. MINUS INDICATES A SHORTAGE OF MATERIAL WITHIN THE DIVISION. |

9

9

Division 1 - LACKEY LANE - EAST OF STRUCTURE

| STATION | REAL STATION | DISTANCE | AREA (SF) | | | | | | INCREMENTAL VOL (CY) (UNADJUSTED) | | | CUMULATIVE VOL (CY) | | |
|----------|--------------|----------|-----------|-------------------------------------|-------|-----------|----------|------|-----------------------------------|-------------------------------------|------|---------------------|--------|---------------|
| | | | CUT | SALVAGED/UNUSABLE PAVEMENT MATERIAL | FILL | MARSH EXC | ROCK EXC | EBS | CUT | SALVAGED/UNUSABLE PAVEMENT MATERIAL | FILL | EXPANDED | | MASS ORDINATE |
| | | | | | | | | | | | | NOTE 1 | NOTE 2 | |
| 22+18.00 | 2218 | 0.00 | 16.25 | 5.67 | 6.40 | 0.00 | 0.00 | 0.00 | 0 | 0 | 0 | 0 | 0 | 0 |
| 22+20.00 | 2220 | 2.00 | 16.41 | 5.67 | 5.17 | 0.00 | 0.00 | 0.00 | 1 | 0 | 0 | 1 | 0 | 1 |
| 22+25.00 | 2225 | 5.00 | 18.09 | 5.67 | 17.19 | 0.00 | 0.00 | 0.00 | 3 | 1 | 2 | 4 | 2 | 1 |
| 22+30.00 | 2230 | 5.00 | 20.81 | 5.67 | 9.13 | 0.00 | 0.00 | 0.00 | 4 | 1 | 2 | 8 | 4 | 2 |
| 22+35.00 | 2235 | 5.00 | 20.57 | 5.67 | 8.65 | 0.00 | 0.00 | 0.00 | 4 | 1 | 2 | 12 | 7 | 2 |
| 22+40.00 | 2240 | 5.00 | 21.03 | 5.67 | 8.39 | 0.00 | 0.00 | 0.00 | 4 | 1 | 2 | 16 | 9 | 3 |
| 22+45.00 | 2245 | 5.00 | 21.51 | 5.67 | 7.32 | 0.00 | 0.00 | 0.00 | 4 | 1 | 1 | 20 | 10 | 5 |
| 22+50.00 | 2250 | 5.00 | 21.43 | 5.67 | 6.52 | 0.00 | 0.00 | 0.00 | 4 | 1 | 1 | 24 | 11 | 7 |
| 22+55.00 | 2255 | 5.00 | 21.06 | 5.67 | 5.87 | 0.00 | 0.00 | 0.00 | 4 | 1 | 1 | 28 | 12 | 9 |
| 22+60.00 | 2260 | 5.00 | 21.14 | 5.67 | 5.32 | 0.00 | 0.00 | 0.00 | 4 | 1 | 1 | 32 | 13 | 11 |
| 22+65.00 | 2265 | 5.00 | 21.61 | 5.67 | 4.85 | 0.00 | 0.00 | 0.00 | 4 | 1 | 1 | 36 | 14 | 13 |
| 22+70.00 | 2270 | 5.00 | 22.17 | 5.67 | 4.31 | 0.00 | 0.00 | 0.00 | 4 | 1 | 1 | 40 | 15 | 15 |
| 22+75.00 | 2275 | 5.00 | 22.72 | 5.67 | 3.79 | 0.00 | 0.00 | 0.00 | 4 | 1 | 1 | 44 | 17 | 17 |
| 22+80.00 | 2280 | 5.00 | 23.27 | 5.67 | 3.30 | 0.00 | 0.00 | 0.00 | 4 | 1 | 1 | 48 | 18 | 18 |
| 22+85.00 | 2285 | 5.00 | 23.84 | 5.67 | 2.82 | 0.00 | 0.00 | 0.00 | 4 | 1 | 1 | 52 | 19 | 20 |
| 22+90.00 | 2290 | 5.00 | 24.37 | 5.67 | 2.43 | 0.00 | 0.00 | 0.00 | 4 | 1 | 0 | 56 | 19 | 23 |
| 22+95.00 | 2295 | 5.00 | 24.85 | 5.67 | 2.10 | 0.00 | 0.00 | 0.00 | 5 | 1 | 0 | 61 | 19 | 27 |
| 23+00.00 | 2300 | 5.00 | 24.84 | 5.67 | 1.58 | 0.00 | 0.00 | 0.00 | 5 | 1 | 0 | 66 | 19 | 31 |
| 23+05.00 | 2305 | 5.00 | 24.78 | 5.67 | 1.06 | 0.00 | 0.00 | 0.00 | 5 | 1 | 0 | 71 | 19 | 35 |
| 23+10.00 | 2310 | 5.00 | 24.55 | 5.67 | 0.55 | 0.00 | 0.00 | 0.00 | 5 | 1 | 0 | 76 | 19 | 39 |
| 23+15.00 | 2315 | 5.00 | 25.10 | 5.67 | 0.06 | 0.00 | 0.00 | 0.00 | 5 | 1 | 0 | 81 | 19 | 43 |
| 23+20.00 | 2320 | 5.00 | 0.00 | 5.67 | 0.00 | 0.00 | 0.00 | 0.00 | 2 | 1 | 0 | 83 | 19 | 44 |

Division 2 - BYPASS ROAD

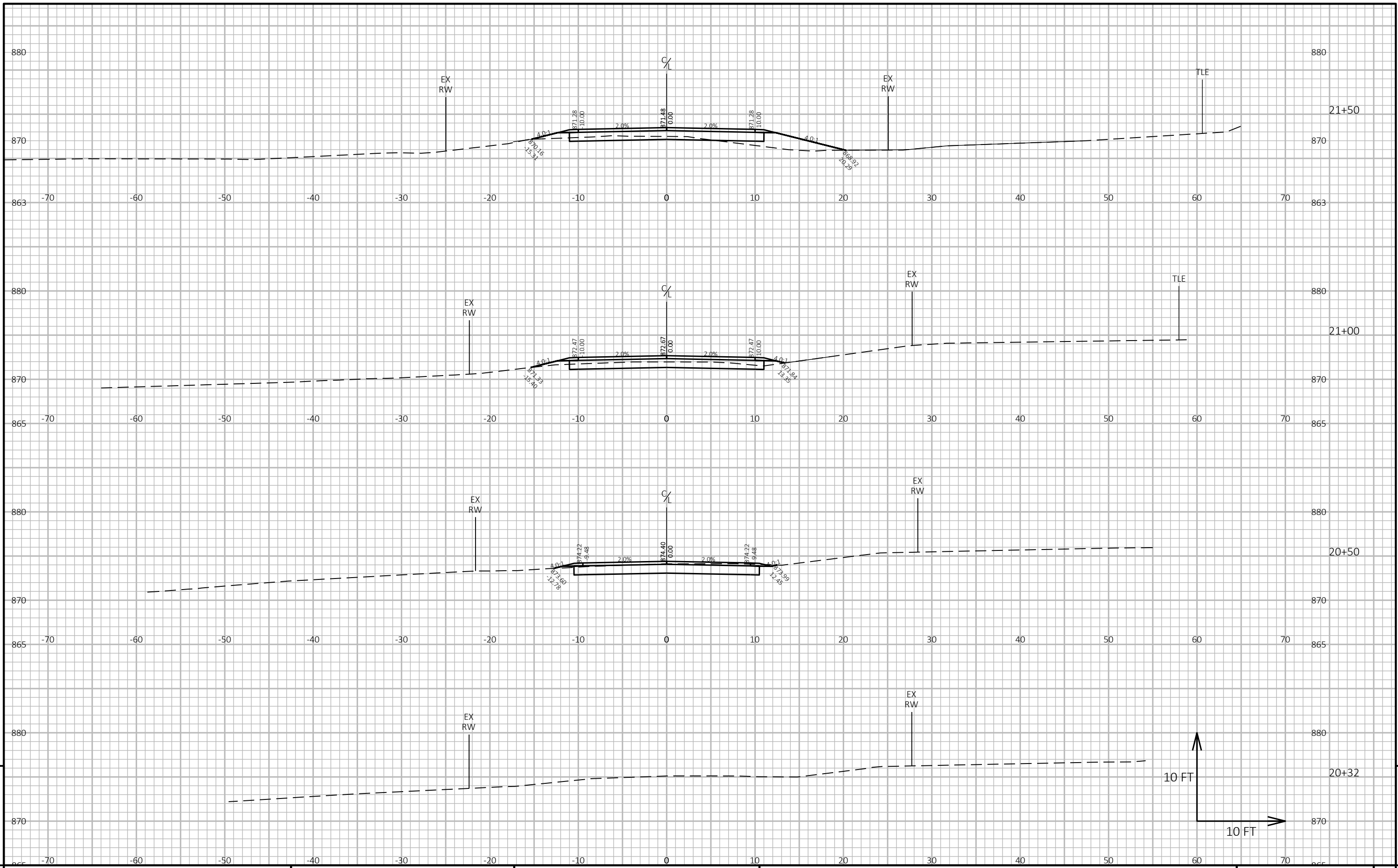
| STATION | REAL STATION | DISTANCE | AREA (SF) | | | | | | INCREMENTAL VOL (CY) (UNADJUSTED) | | | CUMULATIVE VOL (CY) | | |
|----------|--------------|----------|-----------|-------------------------------------|--------|-----------|----------|--------|-----------------------------------|-------------------------------------|--------|---------------------|--------|---------------|
| | | | CUT | SALVAGED/UNUSABLE PAVEMENT MATERIAL | FILL | MARSH EXC | ROCK EXC | EBS | CUT | SALVAGED/UNUSABLE PAVEMENT MATERIAL | FILL | EXPANDED | | MASS ORDINATE |
| | | | | | | | | | | | | 1.00 | 1.10 | |
| NOTE 1 | NOTE 2 | NOTE 3 | NOTE 1 | NOTE 1 | NOTE 1 | NOTE 1 | NOTE 1 | NOTE 2 | NOTE 3 | NOTE 1 | NOTE 1 | NOTE 1 | NOTE 1 | |
| 40+50.00 | 4050 | 0.00 | 0.00 | 0.00 | 0.07 | 0.00 | 0.00 | 0.00 | 0 | 0 | 0 | 0 | 0 | 0 |
| 40+55.00 | 4055 | 5.00 | 0.76 | 0.00 | 0.05 | 0.00 | 0.00 | 0.00 | 0 | 0 | 0 | 0 | 0 | 0 |
| 40+60.00 | 4060 | 5.00 | 1.31 | 0.00 | 0.28 | 0.00 | 0.00 | 0.00 | 0 | 0 | 0 | 0 | 0 | 0 |
| 40+65.00 | 4065 | 5.00 | 2.65 | 0.00 | 0.95 | 0.00 | 0.00 | 0.00 | 0 | 0 | 0 | 0 | 0 | 0 |
| 40+70.00 | 4070 | 5.00 | 4.13 | 0.00 | 2.14 | 0.00 | 0.00 | 0.00 | 1 | 0 | 0 | 1 | 0 | 1 |
| 40+75.00 | 4075 | 5.00 | 6.28 | 0.00 | 2.89 | 0.00 | 0.00 | 0.00 | 1 | 0 | 0 | 2 | 0 | 2 |
| 40+80.00 | 4080 | 5.00 | 7.89 | 0.00 | 3.01 | 0.00 | 0.00 | 0.00 | 1 | 0 | 1 | 3 | 1 | 2 |
| 40+85.00 | 4085 | 5.00 | 8.46 | 0.00 | 2.49 | 0.00 | 0.00 | 0.00 | 2 | 0 | 1 | 5 | 2 | 3 |
| 40+90.00 | 4090 | 5.00 | 8.14 | 0.00 | 1.59 | 0.00 | 0.00 | 0.00 | 2 | 0 | 0 | 7 | 2 | 5 |
| 40+95.00 | 4095 | 5.00 | 7.55 | 0.00 | 1.50 | 0.00 | 0.00 | 0.00 | 1 | 0 | 0 | 8 | 2 | 6 |
| 41+00.00 | 4100 | 5.00 | 6.67 | 0.00 | 1.55 | 0.00 | 0.00 | 0.00 | 1 | 0 | 0 | 9 | 2 | 7 |
| 41+05.00 | 4105 | 5.00 | 5.65 | 0.00 | 2.02 | 0.00 | 0.00 | 0.00 | 1 | 0 | 0 | 10 | 2 | 8 |
| 41+10.00 | 4110 | 5.00 | 4.19 | 0.00 | 2.94 | 0.00 | 0.00 | 0.00 | 1 | 0 | 0 | 11 | 2 | 9 |
| 41+15.00 | 4115 | 5.00 | 1.89 | 0.00 | 3.31 | 0.00 | 0.00 | 0.00 | 1 | 0 | 1 | 12 | 3 | 9 |
| 41+20.00 | 4120 | 5.00 | 0.90 | 0.00 | 3.64 | 0.00 | 0.00 | 0.00 | 0 | 0 | 1 | 12 | 4 | 8 |
| 41+25.00 | 4125 | 5.00 | 0.59 | 0.00 | 4.13 | 0.00 | 0.00 | 0.00 | 0 | 0 | 1 | 12 | 6 | 7 |
| 41+30.00 | 4130 | 5.00 | 0.24 | 0.00 | 4.75 | 0.00 | 0.00 | 0.00 | 0 | 0 | 1 | 12 | 7 | 5 |
| 41+35.00 | 4135 | 5.00 | 0.01 | 0.00 | 5.47 | 0.00 | 0.00 | 0.00 | 0 | 0 | 1 | 12 | 8 | 4 |
| 41+40.00 | 4140 | 5.00 | 0.00 | 0.00 | 6.14 | 0.00 | 0.00 | 0.00 | 0 | 0 | 1 | 12 | 9 | 3 |
| 41+45.00 | 4145 | 5.00 | 0.00 | 0.00 | 6.17 | 0.00 | 0.00 | 0.00 | 0 | 0 | 1 | 12 | 10 | 2 |
| 41+50.00 | 4150 | 5.00 | 0.00 | 0.00 | 5.71 | 0.00 | 0.00 | 0.00 | 0 | 0 | 1 | 12 | 11 | 1 |
| 41+55.00 | 4155 | 5.00 | 0.00 | 0.00 | 7.57 | 0.00 | 0.00 | 0.00 | 0 | 0 | 1 | 12 | 12 | 0 |
| 41+60.00 | 4160 | 5.00 | 0.00 | 0.00 | 15.37 | 0.00 | 0.00 | 0.00 | 0 | 0 | 2 | 12 | 14 | -2 |
| 41+65.00 | 4165 | 5.00 | 0.00 | 0.00 | 27.81 | 0.00 | 0.00 | 0.00 | 0 | 0 | 4 | 12 | 19 | -7 |
| 41+70.00 | 4170 | 5.00 | 0.00 | 0.00 | 48.71 | 0.00 | 0.00 | 0.00 | 0 | 0 | 7 | 12 | 26 | -14 |
| 41+75.00 | 4175 | 5.00 | 0.00 | 0.00 | 33.62 | 0.00 | 0.00 | 0.00 | 0 | 0 | 8 | 12 | 35 | -23 |

Division 2 - BYPASS ROAD

| STATION | REAL STATION | DISTANCE | AREA (SF) | | | | | | INCREMENTAL VOL (CY) (UNADJUSTED) | | | CUMULATIVE VOL (CY) | | |
|----------|--------------|----------|-----------|-------------------------------------|--------|-----------|----------|--------|-----------------------------------|-------------------------------------|--------|---------------------|--------|---------------|
| | | | CUT | SALVAGED/UNUSABLE PAVEMENT MATERIAL | FILL | MARSH EXC | ROCK EXC | EBS | CUT | SALVAGED/UNUSABLE PAVEMENT MATERIAL | FILL | EXPANDED | | MASS ORDINATE |
| | | | | | | | | | | | | 1.00 | 1.10 | |
| NOTE 1 | NOTE 2 | NOTE 3 | NOTE 1 | NOTE 1 | NOTE 1 | NOTE 1 | NOTE 1 | NOTE 2 | NOTE 3 | NOTE 1 | NOTE 1 | NOTE 1 | NOTE 1 | |
| 42+25.00 | 4225 | 0.00 | 0.00 | 0.00 | 9.94 | 0.00 | 0.00 | 0.00 | 0 | 0 | 0 | 0 | 0 | 0 |
| 42+30.00 | 4230 | 5.00 | 0.00 | 0.00 | 5.74 | 0.00 | 0.00 | 0.00 | 0 | 0 | 1 | 0 | 1 | -1 |
| 42+35.00 | 4235 | 5.00 | 0.27 | 0.00 | 1.02 | 0.00 | 0.00 | 0.00 | 0 | 0 | 1 | 0 | 2 | -2 |
| 42+40.00 | 4240 | 5.00 | 1.36 | 0.00 | 0.35 | 0.00 | 0.00 | 0.00 | 0 | 0 | 0 | 0 | 2 | -2 |
| 42+45.00 | 4245 | 5.00 | 2.21 | 0.00 | 0.24 | 0.00 | 0.00 | 0.00 | 0 | 0 | 0 | 0 | 2 | -2 |
| 42+50.00 | 4250 | 5.00 | 2.82 | 0.00 | 0.18 | 0.00 | 0.00 | 0.00 | 0 | 0 | 0 | 0 | 2 | -2 |
| 42+55.00 | 4255 | 5.00 | 3.18 | 0.00 | 0.15 | 0.00 | 0.00 | 0.00 | 1 | 0 | 0 | 1 | 2 | -1 |
| 42+60.00 | 4260 | 5.00 | 3.28 | 0.00 | 0.14 | 0.00 | 0.00 | 0.00 | 1 | 0 | 0 | 2 | 2 | 0 |
| 42+65.00 | 4265 | 5.00 | 3.17 | 0.00 | 0.14 | 0.00 | 0.00 | 0.00 | 1 | 0 | 0 | 3 | 2 | 1 |
| 42+70.00 | 4270 | 5.00 | 2.87 | 0.00 | 0.16 | 0.00 | 0.00 | 0.00 | 1 | 0 | 0 | 4 | 2 | 2 |
| 42+75.00 | 4275 | 5.00 | 2.42 | 0.00 | 0.12 | 0.00 | 0.00 | 0.00 | 0 | 0 | 0 | 4 | 2 | 2 |
| 42+80.00 | 4280 | 5.00 | 2.47 | 0.00 | 0.11 | 0.00 | 0.00 | 0.00 | 0 | 0 | 0 | 4 | 2 | 2 |
| 42+85.00 | 4285 | 5.00 | 2.27 | 0.00 | 0.18 | 0.00 | 0.00 | 0.00 | 0 | 0 | 0 | 4 | 2 | 2 |
| 42+90.00 | 4290 | 5.00 | 1.10 | 0.00 | 0.49 | 0.00 | 0.00 | 0.00 | 0 | 0 | 0 | 4 | 2 | 2 |
| 42+95.00 | 4295 | 5.00 | 1.07 | 0.00 | 1.08 | 0.00 | 0.00 | 0.00 | 0 | 0 | 0 | 4 | 2 | 2 |
| 43+00.00 | 4300 | 5.00 | 1.00 | 0.00 | 1.44 | 0.00 | 0.00 | 0.00 | 0 | 0 | 0 | 4 | 2 | 2 |

9

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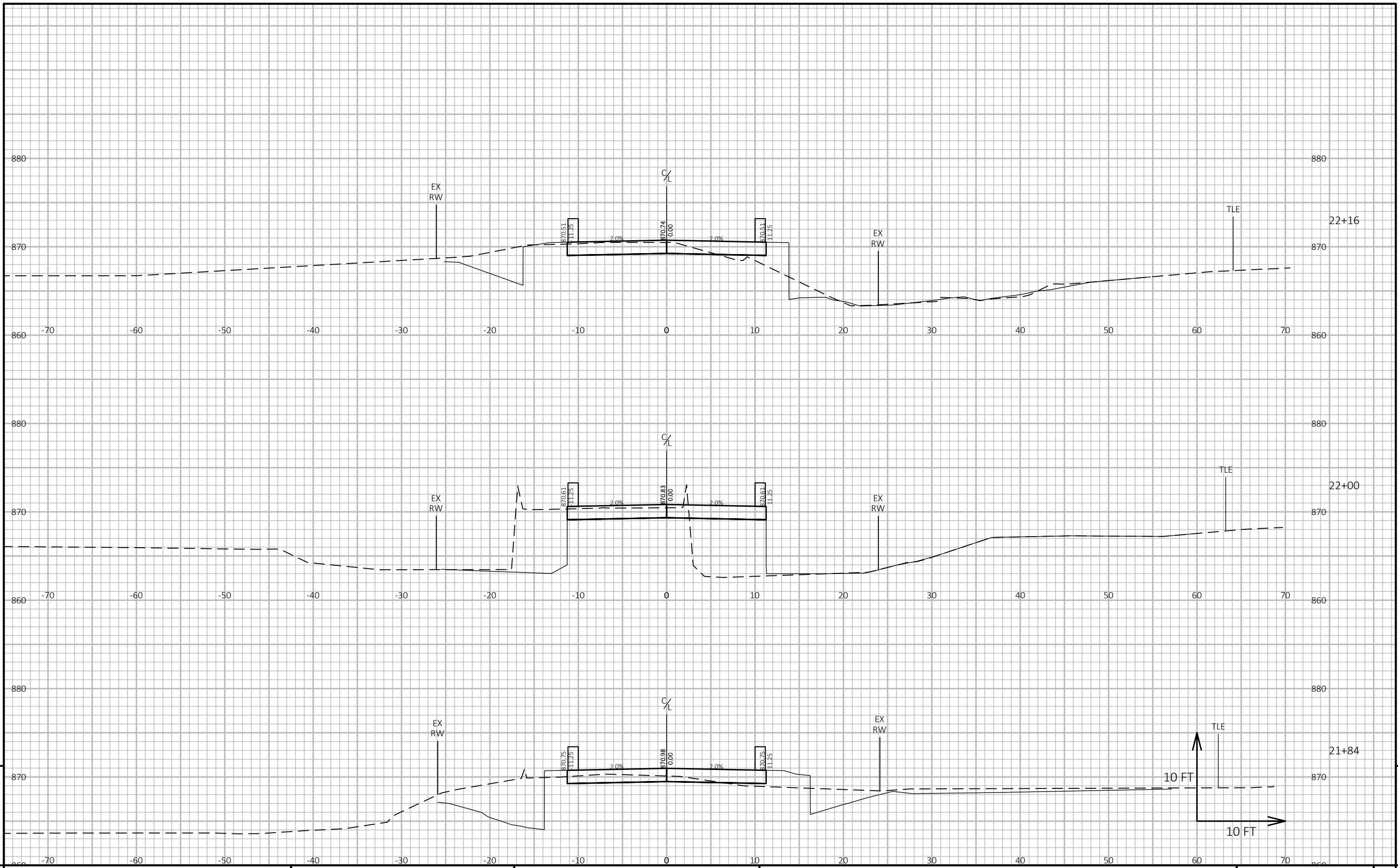


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PROJECT NO: 3846-00-75 HWY: LACKEY LANE COUNTY: WALWORTH CROSS SECTIONS: LACKEY LANE SHEET E

FILE NAME: P:\LINNT\0222470-LACKEY LN BRIDGE\CAD\01_CIVIL 3D-2022\222471-LACKEYLN BRIDGE\SHEETS\090202-XS.DWG PLOT DATE: 4/16/2024 11:03 AM PLOT BY: ELA LODZINSKI PLOT NAME: PLOT SCALE: 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49



PROJECT NO: 3846-00-75

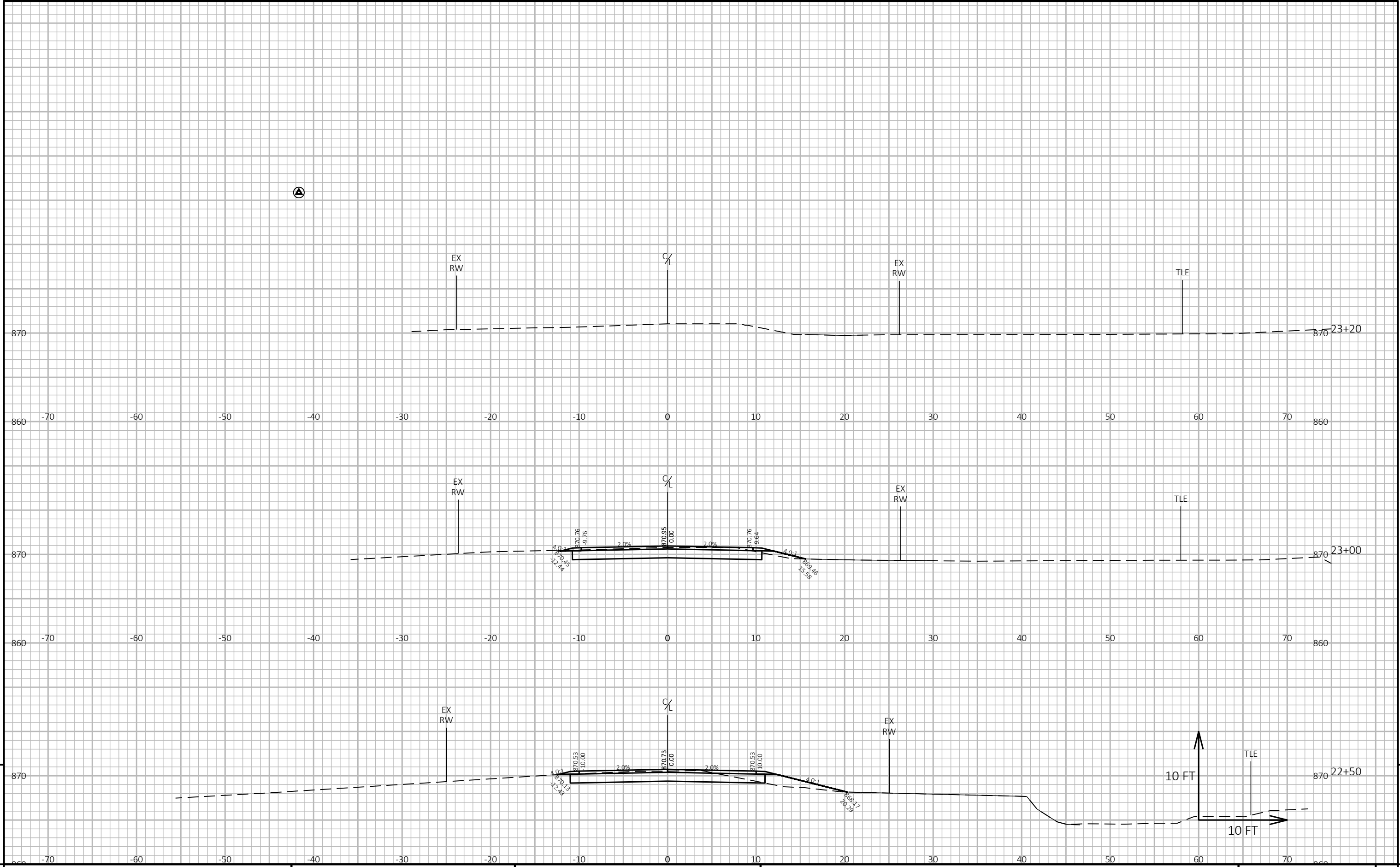
HWY: LACKEY LANE

COUNTY: WALWORTH

CROSS SECTIONS: LACKEY LANE

SHEET

E



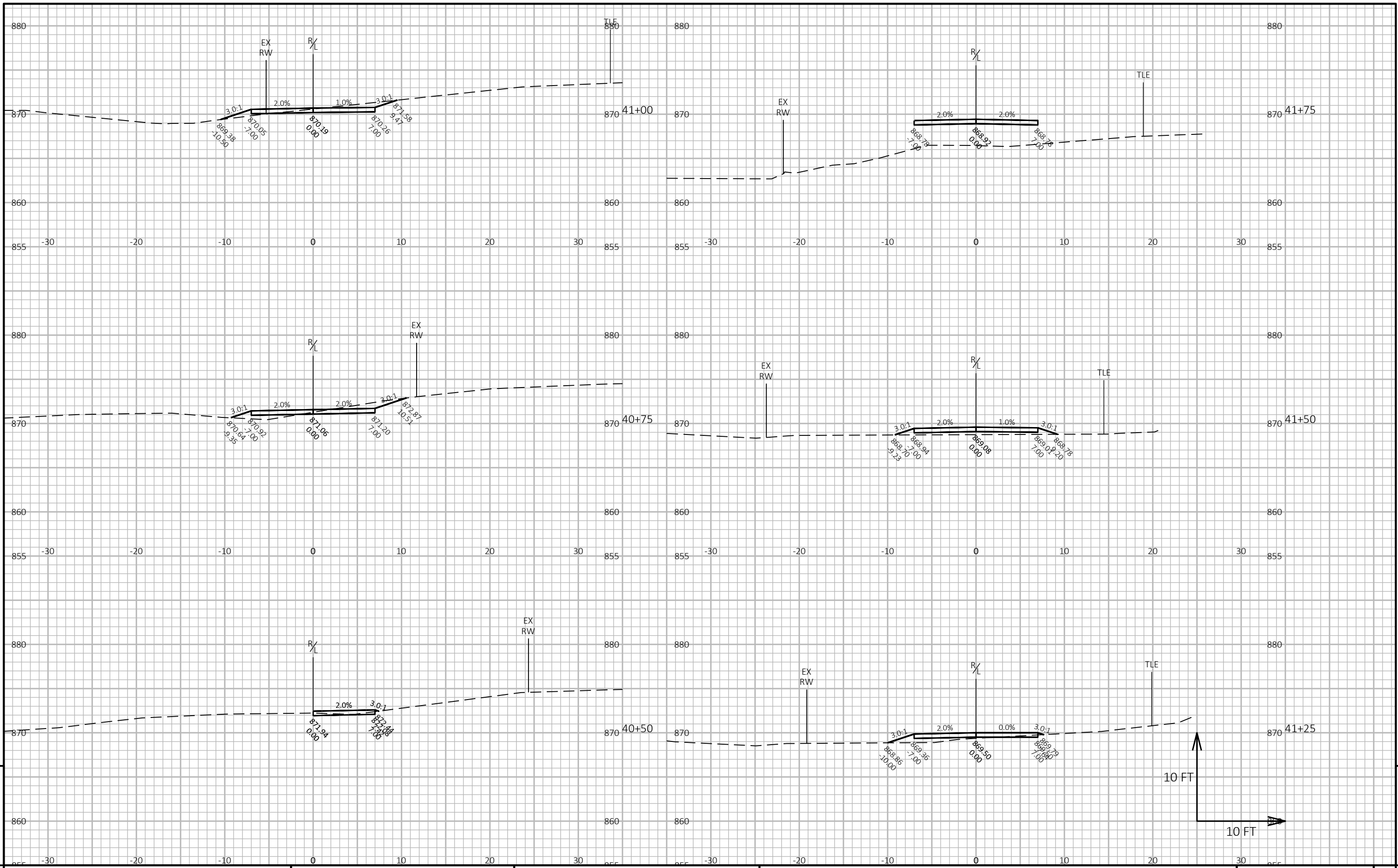
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PROJECT NO: 3846-00-75 HWY: LACKEY LANE COUNTY: WALWORTH CROSS SECTIONS: LACKEY LANE SHEET E

FILE NAME: P:\LINNT\0222470-LACKEY LN BRIDGE\CAD\01_CIVIL 3D-2022\222471-LACKEYLN BRIDGE\SHEETS\090202-XS.DWG PLOT DATE: 4/16/2024 11:03 AM PLOT BY: ELA LODZINSKI PLOT NAME: PLOT SCALE: 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49

LAYOUT NAME - 090202-(3)



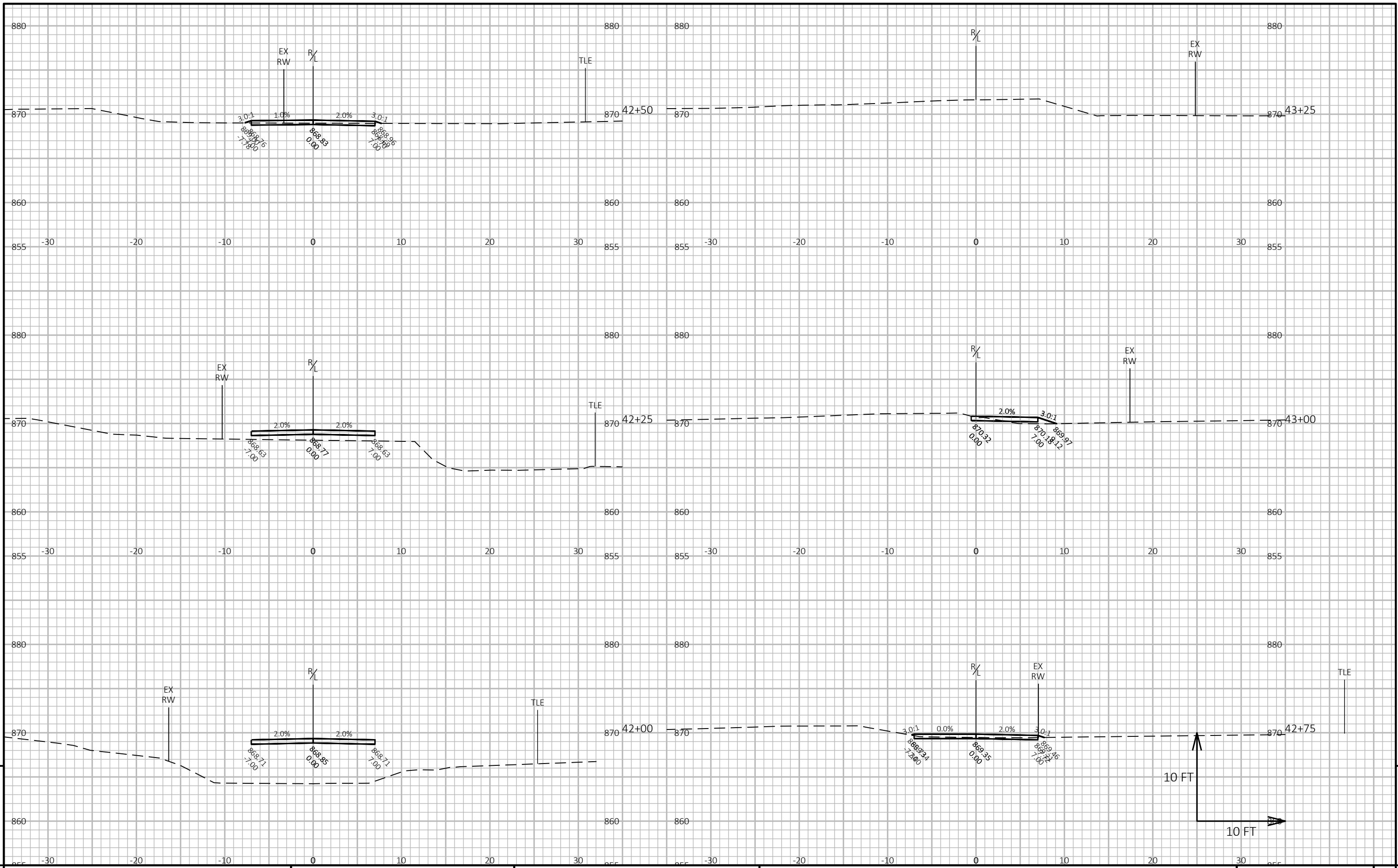
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PROJECT NO: 3846-00-75 HWY: LACKEY LANE COUNTY: WALWORTH CROSS SECTIONS: TEMPORARY BYPASS SHEET E

FILE NAME : P:\LINN\0222470-LACKEY LN BRIDGE\CAD\01_CIVIL 3D-2022\222471-LACKEYLN BRIDGE\SHEETS\090202-XS.DWG PLOT DATE : 4/16/2024 11:03 AM PLOT BY : ELA LODZINSKI PLOT NAME : PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49

LAYOUT NAME - 090203-(1)



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9

PROJECT NO: 3846-00-75 HWY: LACKEY LANE COUNTY: WALWORTH CROSS SECTIONS: TEMPORARY BYPASS SHEET E

FILE NAME : P:\LINN\0222470-LACKEY LN BRIDGE\CAD\01_CIVIL 3D-2022\222471-LACKEYLN BRIDGE\SHEETS\090202-XS.DWG PLOT DATE : 4/16/2024 11:03 AM PLOT BY : ELA LODZINSKI PLOT NAME : PLOT SCALE : 1 IN:10 FT HORZ. / 1 IN:10 FT VERT. WISDOT/CADD SHEET 49

LAYOUT NAME - 090203-(2)



Wisconsin Department of Transportation

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